

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM: 3.1

HEARING DATE: June 8, 2023

CASE NUMBER: ZAP1570MA23 - Hillwood (Representative: Albert A. Webb & Associates)

APPROVING JURISDICTION: City of Perris

JURISDICTION CASE NO: GPA22-0532 (General Plan Amendment), ZC22-05327 (Change of Zone), DPR22-00030 (Development Plan Review), TPM22-05328 (Parcel Map)

LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

Airport Influence Area: March Air Reserve Base/Inland Port Airport

Land Use Policy: Compatibility Zone D

Noise Levels: Below 60 CNEL from aircraft

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends that the Commission find the General Plan Amendment and Change of Zone CONSISTENT with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, and also find the Development Plan Review and Tentative Parcel Map CONSISTENT, subject to the conditions included herein.

PROJECT DESCRIPTION: A proposal to construct a 412,348 square foot manufacturing building with mezzanines on 20 acres. The applicant also proposes to amend the General Plan Land use designation and change the sites zoning from Commercial to Light Industrial. The applicant also proposes merging eight existing parcels into one.

PROJECT LOCATION: The site is located northerly of Ethanac Road, westerly of Sherman Road, and easterly of Trumble Road, approximately 47,670 feet southeasterly of the southerly end of Runway 14-32 at March Air Reserve Base, and 10,854 feet southeasterly of the southerly end of Runway 15-33 at Perris Valley Airport.

BACKGROUND:

Non-Residential Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zone D, which does not restrict non-residential intensity.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zone D.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being outside the 60 CNEL range from aircraft noise. Therefore, no special measures are required to mitigate aircraft-generated noise.

Part 77: Although the project is located within the March Air Reserve Base/Inland Port Airport Influence Area, the nearest runway is actually Runway 15-33 at Perris Valley Airport. The elevation of Runway 15-33 at Perris Valley Airport is approximately 1,413 feet above mean sea level (AMSL) at its southerly terminus. At a distance of 10,854 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with an elevation at top of roof exceeding 1,522 feet AMSL. The site elevation is 1,433 feet AMSL, and the proposed building height is 52 feet, resulting in a top point elevation of 1,485 feet AMSL. Therefore, FAA OES review for height/elevation was not required.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33C). The project is located 10,854 feet from the runway, and therefore would not be subject to the above requirement.

General Plan Amendment and Change of Zone: The applicant also proposes to amend the General Plan Land use designation and change the sites zoning from Commercial to Light Industrial. The proposed amendments would be as, or more, consistent with the Compatibility Plan as the underlying compatibility zone does not restrict intensity.

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight or circling climb following takeoff or toward an aircraft engaged in a straight or circling final approach toward a landing at an airport, other than a DoD or FAA-approved navigational signal light or visual approach slope indicator.

- (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight or circling climb following takeoff or towards an aircraft engaged in a straight or circling final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor, or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Hazards to flight
3. The attached notice shall be provided to all prospective purchasers of the property and tenants of the building, and shall be recorded as a deed notice.
 4. Any proposed detention basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the detention basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

5. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.

6. The project has been evaluated to construct 412,348 square foot manufacturing building with mezzanines. Any increase in building area, change in use to any higher intensity use, change in building location, or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.

X:\AIRPORT CASE FILES\March\ZAP1570MA23\ZAP1570MA23sr.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS

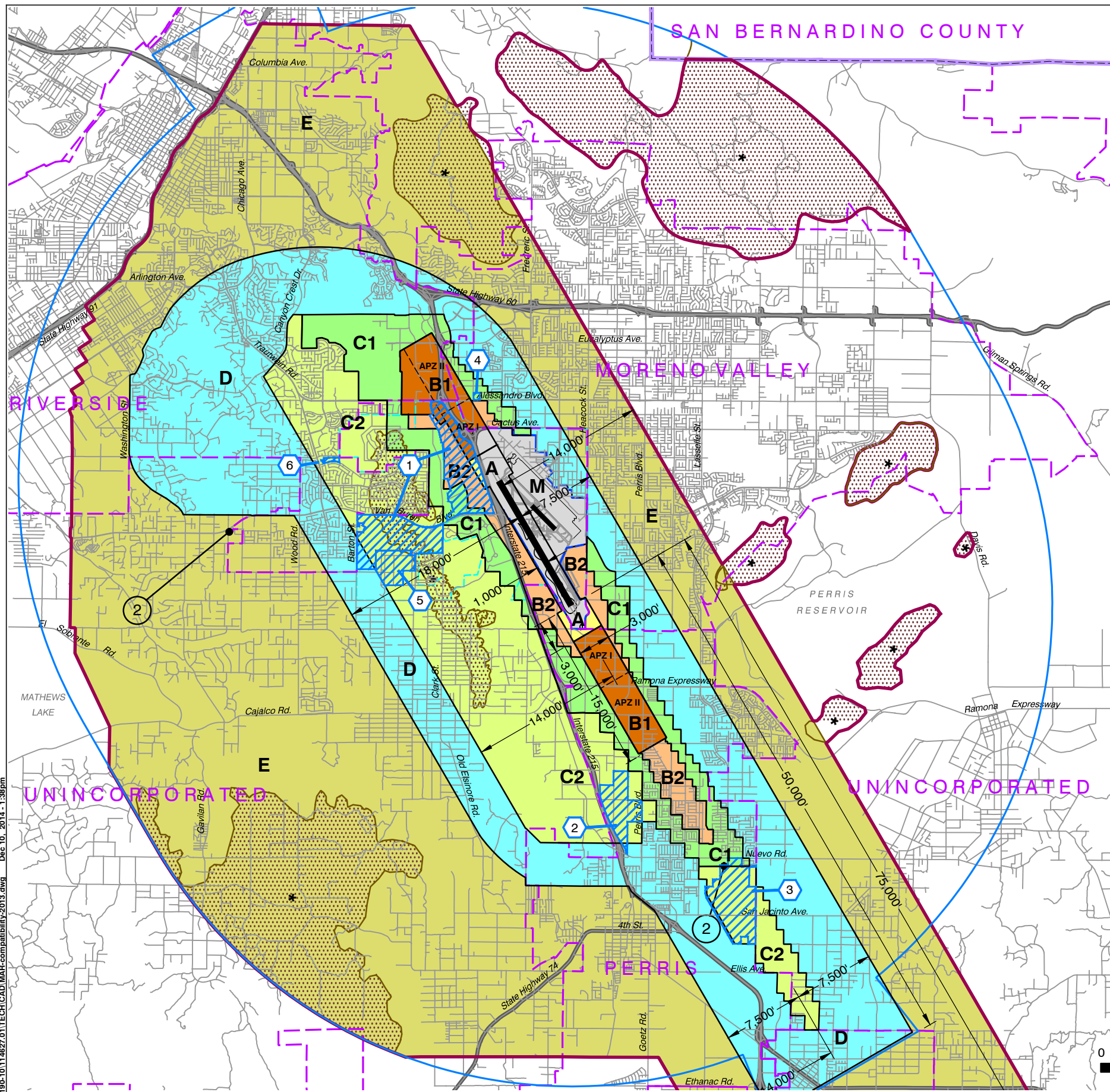
PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



LEGEND

Compatibility Zones

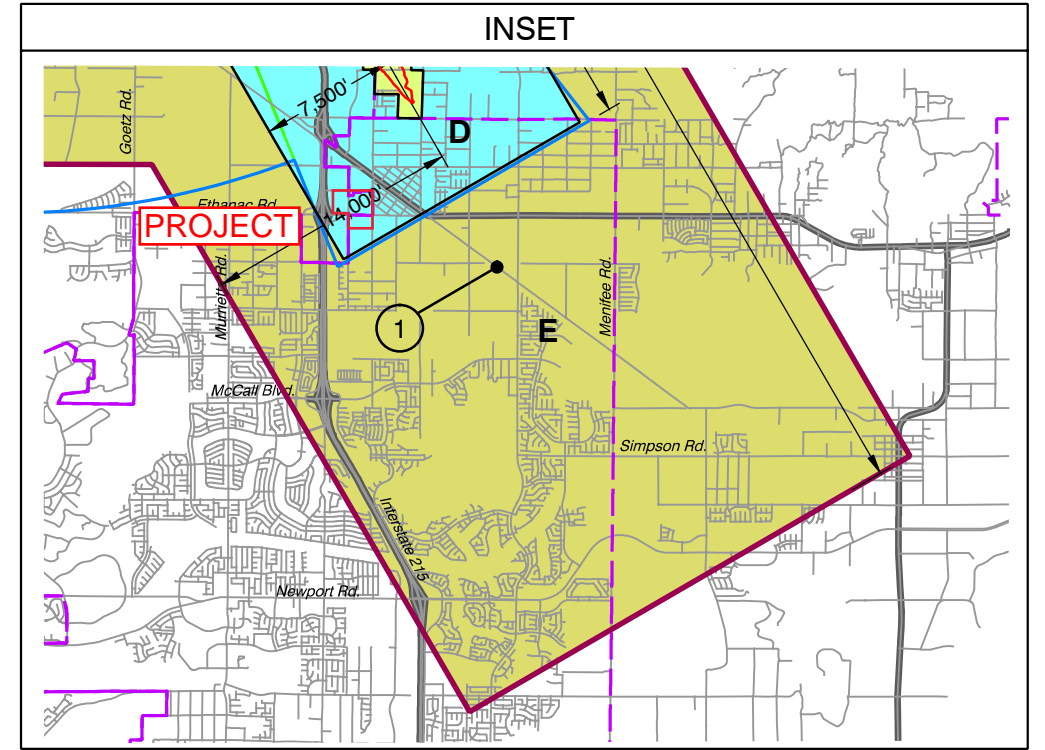
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

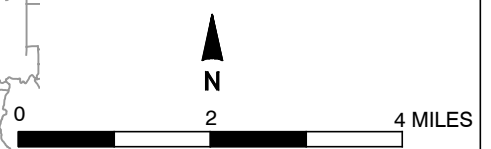
- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



**Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)**

Note:
All dimensions are measured from runway ends and centerlines.



Base map source: County of Riverside 2013

SEE INSET AT RIGHT

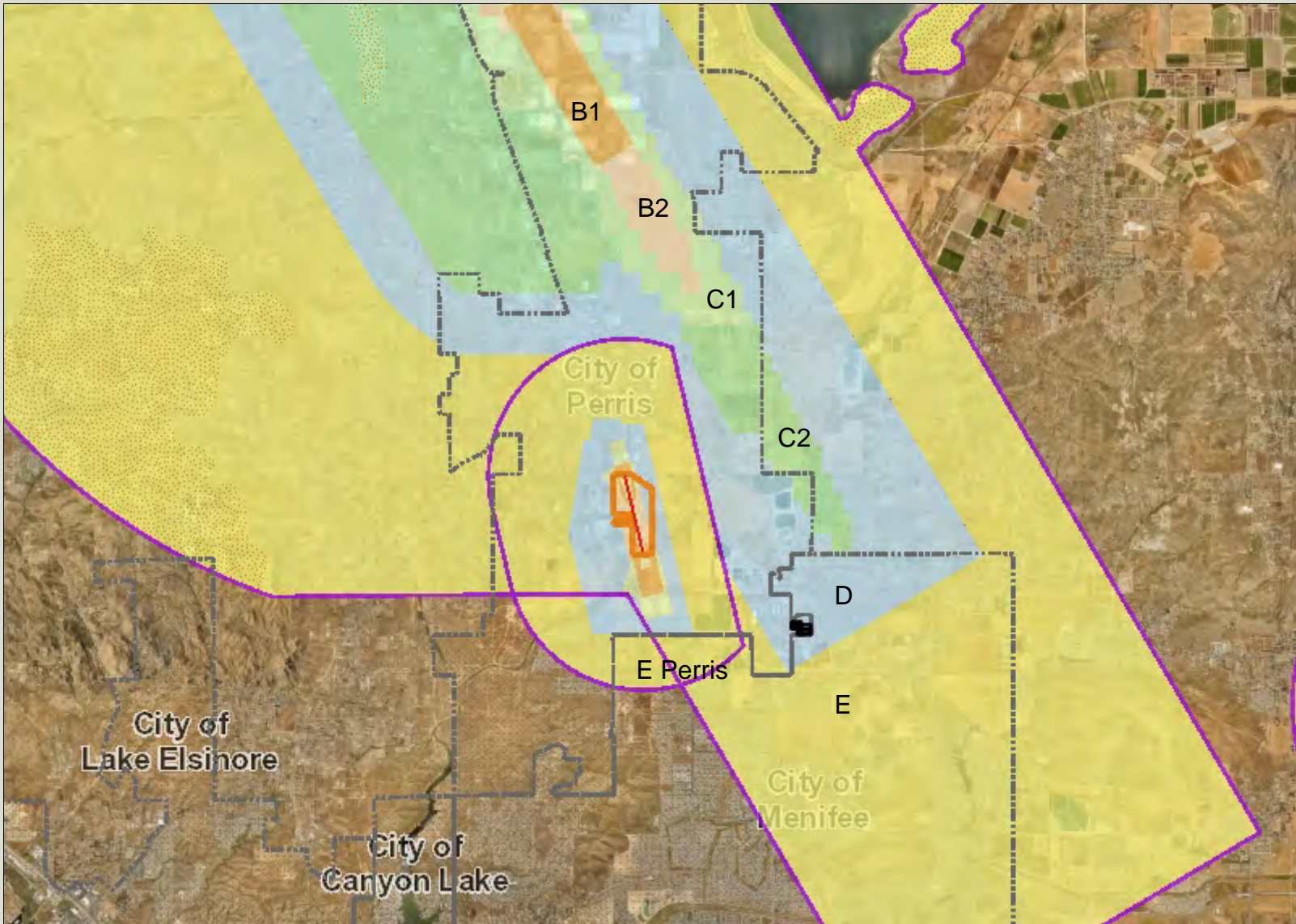
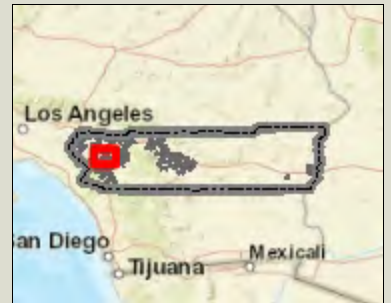
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Prepared by Mead & Hunt, Inc. (June 2013)

Map MA-1

**Compatibility Map
March Air Reserve Base / Inland Port Airport**

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

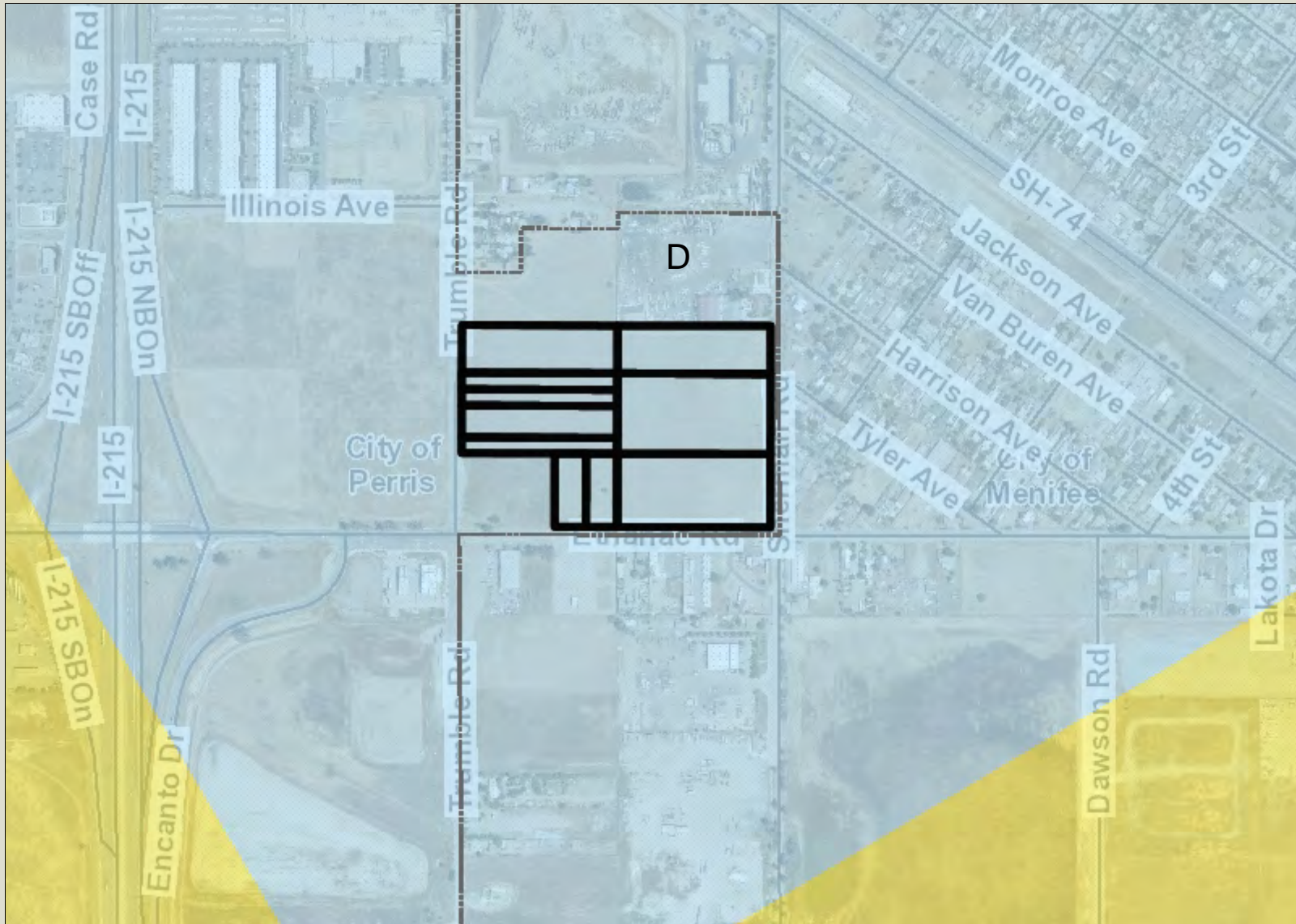


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Notes

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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0 770 1,539 Feet

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Notes

Map My County Map



Legend

-  Parcels
-  County Centerline Names
-  County Centerlines
-  Blueline Streams
-  City Areas
-  World Street Map



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0 376 752 Feet

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Notes

Map My County Map



- Legend**
- County Centerline Names
 - County Centerlines
 - Blueline Streams
 - City Areas
 - World Street Map



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Map My County Map



Legend

-  Parcels
-  County Centerline Names
-  County Centerlines
-  Blueline Streams
-  City Areas
-  World Street Map



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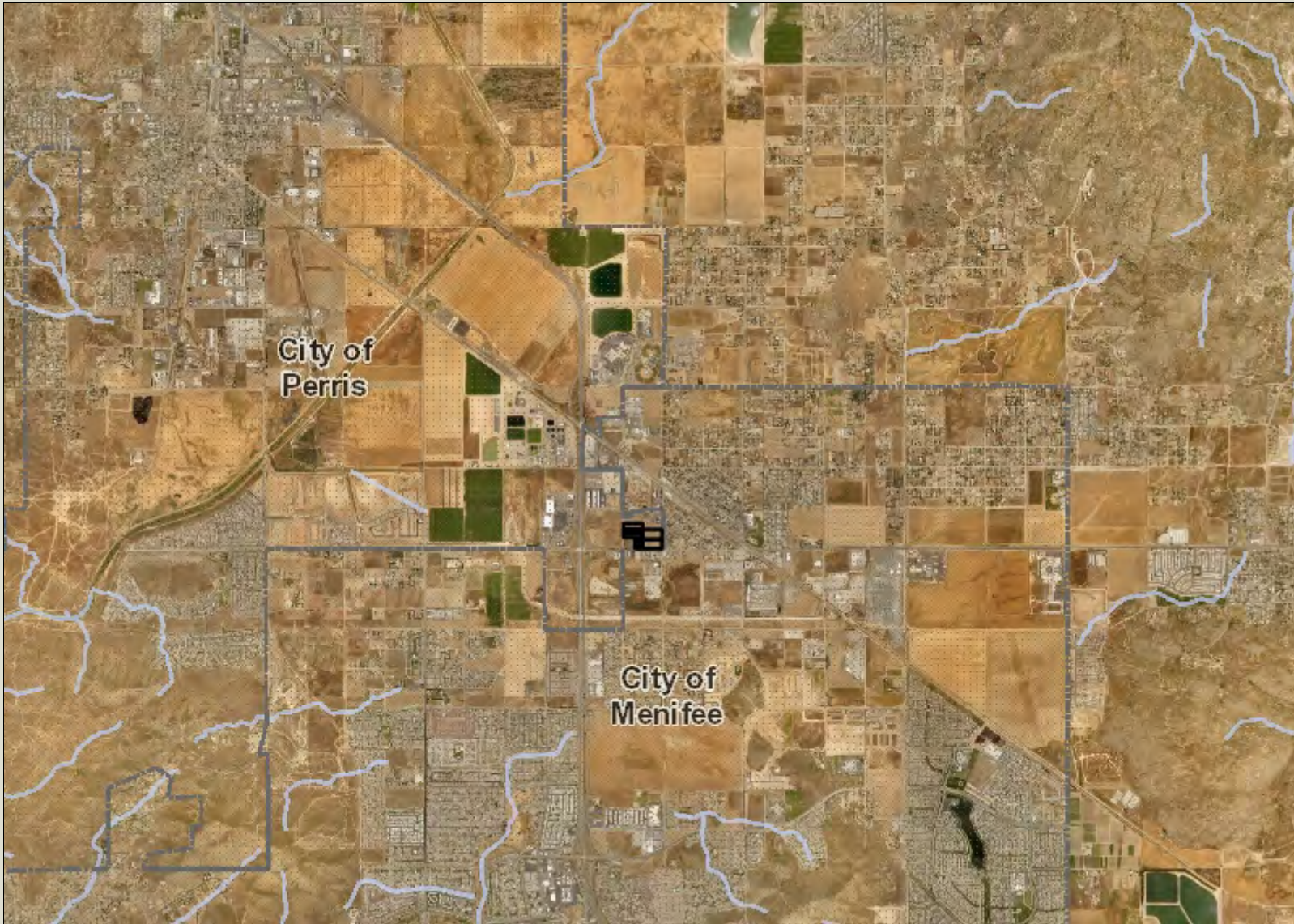
0 385 770 Feet

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Notes

Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



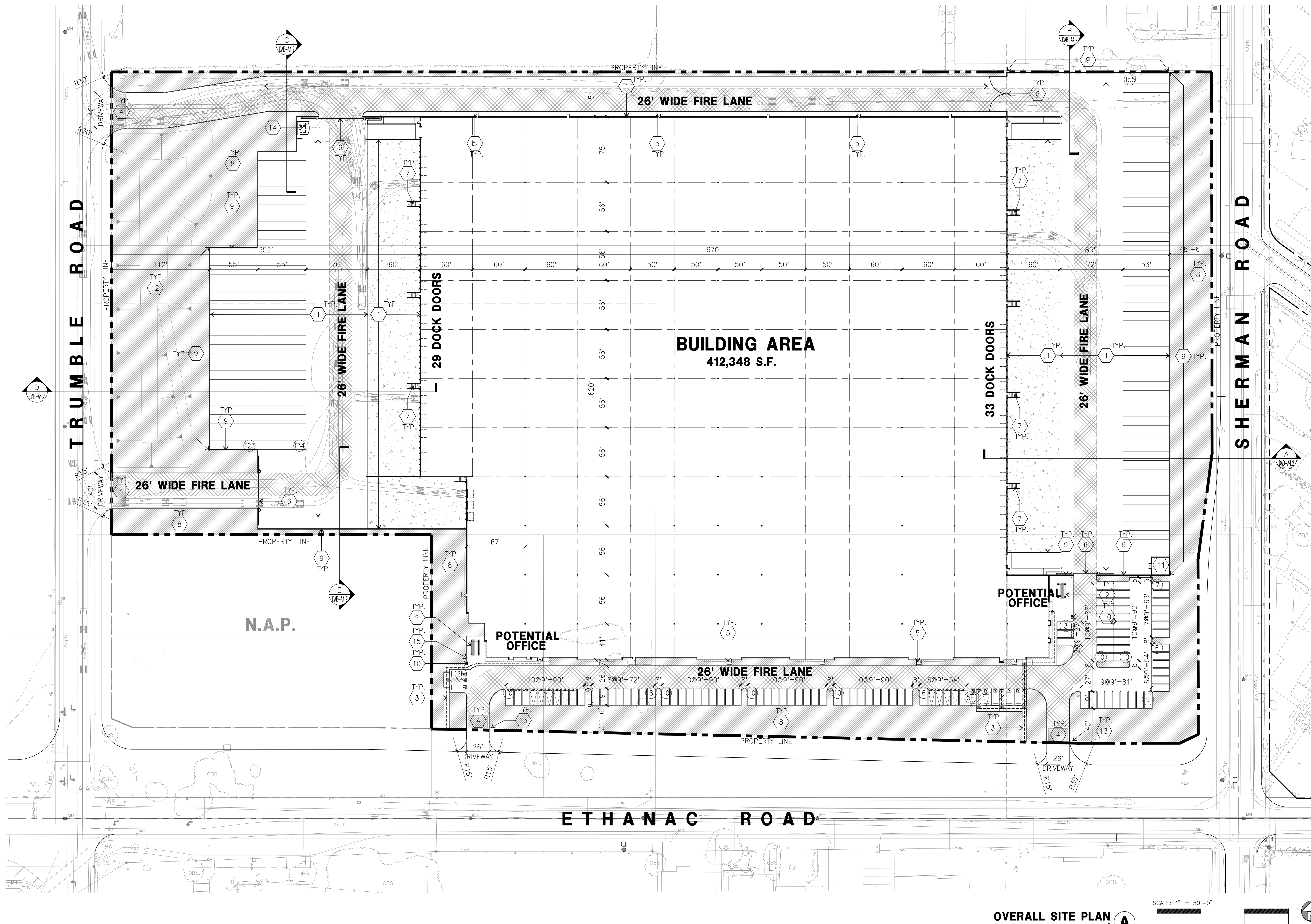
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Notes



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SITE PLAN KEYNOTES

- 1 HEAVY BROOM FINISH CONC. PAVEMENT.
- 2 EMPLOYEE BREAK AREA
- 3 CONCRETE WALKWAY, MEDIUM BROOM FINISH.
- 4 DRIVEWAY APRONS TO BE CONSTRUCTED PER "C" AND "L" DRAWINGS.
- 5 5'-6"x5'-6"x4" MIN. THICK CONCRETE EXTERIOR LANDING PAD TYP. AT ALL EXTERIOR MAN DOORS TO LANDSCAPED AREAS. FINISH TO BE MEDIUM BROOM FINISH. SLOPE TO BE 1/4" : 12" MAX.
- 6 8' H PROVIDE METAL, MANUAL OPERATED GATES W/ VIEW OBSCURING MESH, KNOX-PAD LOCK PER FIRE DEPARTMENT STANDARDS PER DRIVEWAY.
- 7 EXTERIOR CONC. STAIR.
- 8 LANDSCAPE, NOTED BY SHADING. SEE "L" DWGS.
- 9 14" CONCRETE TILT-UP SCREEN WALL W/ DECORATIVE PILASTERS EVERY 100'.
- 10 BIKE RACK
- 11 PROPOSED PUMPHOUSE LOCATION
- 12 DETENTION BASIN. SEE "C" DWGS.
- 13 ACCESSIBLE ENTRY SIGN
- 14 TRASH ENCLOSURE PER CITY STANDARD
- 15 DESIGNATED SMOKER'S AREA

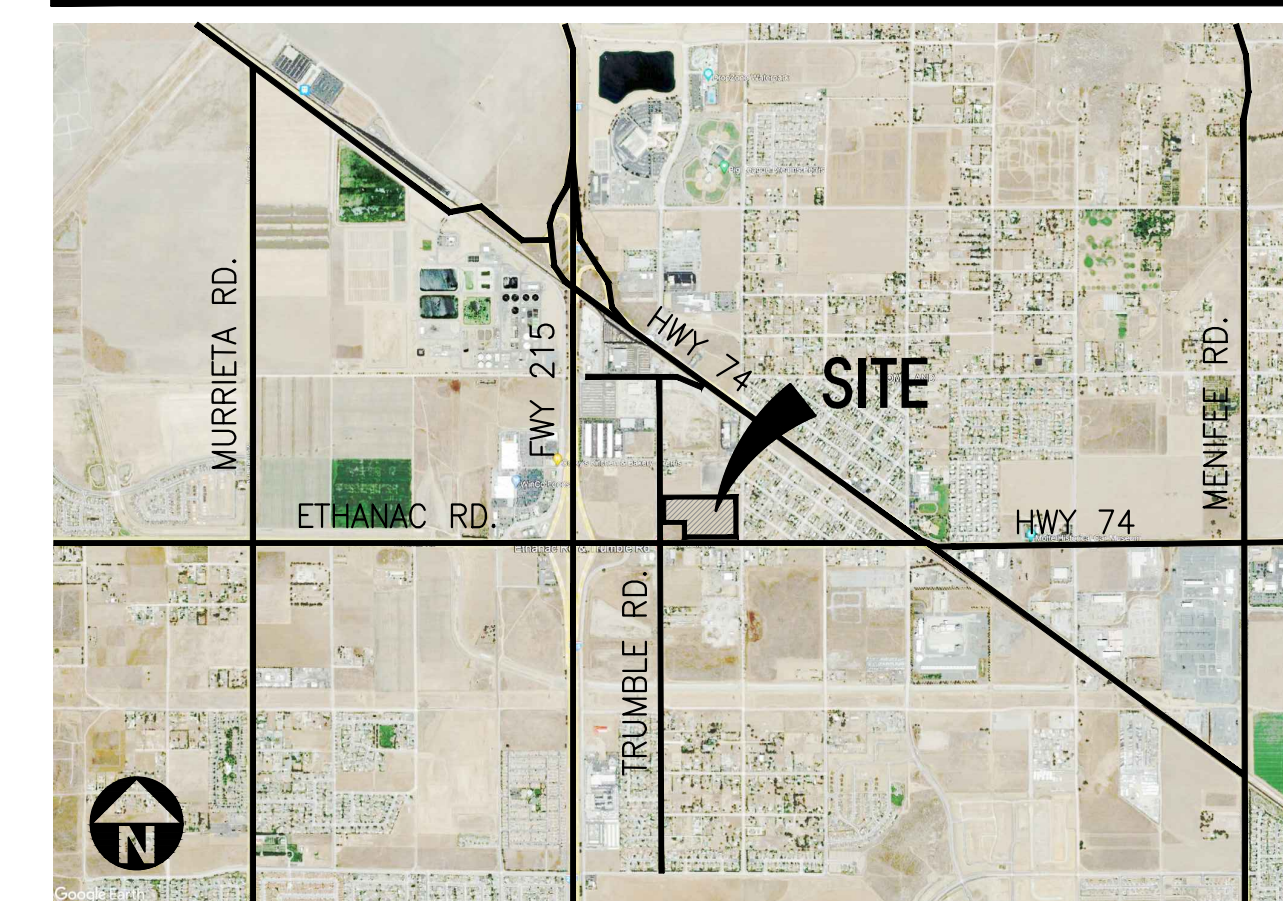
SITE PLAN GENERAL NOTES

1. THE SITE PLAN BASED ON THE SOILS REPORT PREPARED BY
2. IF SOILS ARE EXPANSIVE IN NATURE, USE STEEL REINFORCING FOR ALL SITE CONCRETE.
3. ALL DIMENSIONS ARE TO THE FACE OF CONCRETE WALL, FACE OF CONCRETE CURB OR GRID LINE UNL.O.
4. SEE "C" PLANS FOR ALL CONCRETE CURBS, GUTTERS AND SWALES
5. PROVIDE STRUCTURAL CALCULATION AND CONSTRUCTION AND/OR HOGE DETAIL FOR TRANSFORMER PRIOR TO INSTALLATION.
6. SEE "C" DRAWINGS FOR POINT OF CONNECTIONS TO OFF-SITE UTILITIES. CONTRACTOR SHALL VERIFY ACTUAL UTILITY LOCATIONS.
7. PROVIDE POSITIVE DRAINAGE AWAY FROM BLDG. SEE "C" DRAWINGS.
8. CONTRACTOR TO REFER TO "C" DRAWINGS FOR ALL HORIZONTAL CONTROL DIMENSIONS. SITE PLANS ARE FOR GUIDANCE AND STARTING LAYOUT POINTS.
9. SEE "C" DRAWINGS FOR FINISH GRADE ELEVATIONS.
10. CONCRETE SIDEWALKS TO BE A MINIMUM OF 4" THICK W/ TOOLED JOINTS AT 6' O.C. EXPANSION/CONSTRUCTION JOINTS SHALL BE A MAXIMUM 12' EA. WAY W/ 1:20 MAX. SLOPE. EXPANSION JOINTS TO HAVE COMPRESSIVE EXPANSION FILLER MATERIAL OF 1/4", FINISH TO BE A MEDIUM BROOM FINISH UNL.O. PROVIDE KNOX BOXES AT ALL OFFICE ENTRANCES.
12. PAINT CURBS AND PROVIDE SIGNS TO INFORM OF FIRE LANES AS REQUIRED BY FIRE DEPARTMENT.
13. ON-SITE FIRE MAIN, FIRE SPRINKLER, AND SPRINKLER MONITORING SYSTEM SHALL BE SUBMITTED SEPARATELY TO THE FIRE DEPARTMENT FOR REVIEW AND PERMITTING.
14. ALL VERTICAL MOUNTING POLES OF FENCING SHALL BE CAPPED.
15. LANDSCAPED AREAS SHALL BE DELINEATED WITH A MINIMUM SIX INCHES (6") HIGH CURB
16. ALL INTERIOR AND EXTERIOR WALK SURFACES TO BE NON-SLIP TYPE

SITE PLAN GENERAL NOTES

- CONCRETE PAVING, SEE "C" DRWS. FOR THICKNESS
- STANDARD PARKING STALL (9' x 19')
- CLEAN AIR/VANPOOL/EV CONDUIT STUB FOR FUTURE EV
- CLEAN AIR/VANPOOL/EV WITHOUT CONDUIT STUB FOR FUTURE EV
- TRAILER PARKING (10' x 53')
- LANDSCAPED AREA
- NON-ACCESSIBLE PATH
- ACCESSIBLE PARKING STALL (9' x 19') + 5' W/ ACCESSIBLE AISLE
- ACCESSIBLE PARKING (VAN) STALL (12'-0" x 19') + 5' W/ ACCESSIBLE AISLE
- PATH OF TRAVEL MINIMUM WIDTH TO BE 4'. SLOPE NOT TO EXCEED 2% IN THE DIRECTION OF TRAVEL AND CROSS SLOPE NOT TO EXCEED 2%. SEE CIVIL FOR GRADING PLAN
- PROPERTY LINE
- 26' WIDE FIRE LANE FIRE DEPT STANDARD

VICINITY MAP



PROPERTY OWNER

HILLWOOD
901 VIA PIEMONTE, STE 175
ONTARIO, CA 91764
TEL: 909-256-5924
ATTN: JOHN GRACE

ADDRESS OF THE PROPERTY

TBD
NEC ETHANAC ROAD AND TRUMBLE ROAD

ASSESSOR'S PARCEL NUMBERS

329-240-016 to 20, 23 to 27

ZONING

CURRENT: COMMERCIAL COMMUNITY (CC)
PROPOSED: LIGHT INDUSTRIAL (LI)

LEGAL DESCRIPTION

SEE CIVIL DRAWINGS

APPLICANT

HILLWOOD
901 VIA PIEMONTE, STE 175
ONTARIO, CA 91764
TEL: 909-256-5924
ATTN: JOHN GRACE

ARCHITECT

HPA, INC.
18831 BARDEEN AVE SUITE 100
IRVINE, CA 92612
TEL: 949-862-2122
ATTN: INKON KIM

CIVIL

WEBB ASSOCIATES
3788 CMCRAY STREET
RIVERSIDE, CA 92506
TEL: 951-320-6988
ATTN: OSCAR VALDEZ

PROJECT DATA

SITE AREA	
In s.f.	864,675 sf
In acres	19.9 ac
BUILDING AREA	
Office - 1st floor	10,000 sf
Office - 2nd floor	5,000 sf
Warehouse	397,348 sf
TOTAL	412,348 sf
FLOOR AREA RATIO	
Maximum Allow ed	0.75
Actual	0.477
SITE COVERAGE	
Maximum Allow ed	50%
Actual	47.1%
AUTO PARKING REQUIRED	
High Cuts:	
Office: 1/300 (only if less than 10% GFA)	NA stalls
Whse: 1st 20k @ 1/1,000 s.f.	20 stalls
2nd 20k @ 1/2,000 s.f.	10 stalls
above 40k @ 1/5,000 s.f.	75 stalls
TOTAL	105 stalls
AUTO PARKING PROVIDED	
Standard (9' x 19')	83 stalls
ADA Standard (9' x 19')	3 stalls
ADA Van (12' x 19')	2 stalls
EV Standard (9' x 19')	11 stalls
EV ADA Standard (9' x 19')	1 stalls
EV Van (12' x 19')	1 stalls
Clean Air/Vanpool/EV (9' x 19')	5 stalls
TOTAL	106 stalls
TRAILER PARKING REQUIRED	
Trailer: 1/5,000 s.f.	82 stalls
TRAILER PARKING PROVIDED	
Trailer (10' x 55')	112 stalls
ZONING ORDINANCE	
Current Zoning - Commercial Community	
Proposed Zoning - Light Industrial (LI)	
MAXIMUM BUILDING HEIGHT ALLOWED	
Height - 50'	
LANDSCAPE REQUIREMENT	
Percentage - 15% of the site area	
LANDSCAPE PROVIDED	
In s.f.	142,910 sf
Percentage	16.5%
SETBACKS	
Building	Landscape
Ethanac Rd. - 15' (Arterial)	15'
Trumble Rd. - 10' (Collector)	10'
Sherman Rd. - 10' (Collector)	10'
Side / rear - 0', adjoining R Zone - 20'	
- Loading/unloading - 30'	



hpa, inc.
18831 bardeen avenue, - ste. #100
irvine, ca
92612
tel: 949-863-1770
fax: 949-863-0851
email: hpa@hparchs.com



Hillwood
901 Via Piemonte, Ste 175
Ontario, CA 91764
909-256-5824
ATTN: John Grace

Project:
NEC Ethanac Road & Trumble Road
Perris, CA

Consultants:

CIVIL	WEBB
STRUCTURAL	-
MECHANICAL	-
PLUMBING	-
ELECTRICAL	-
LANDSCAPE	WEBB
FIRE PROTECTION	-
SOILS ENGINEER	-

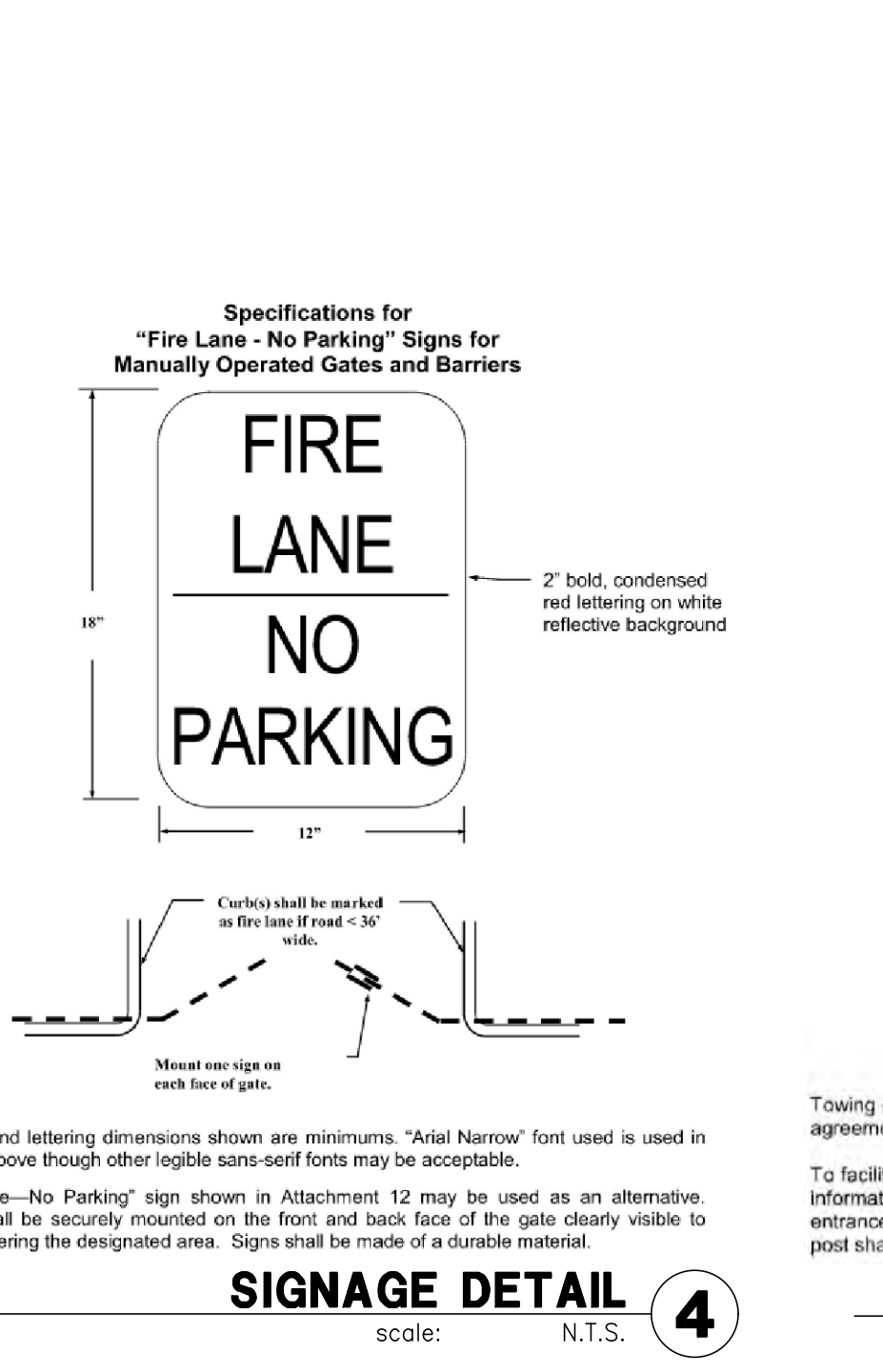
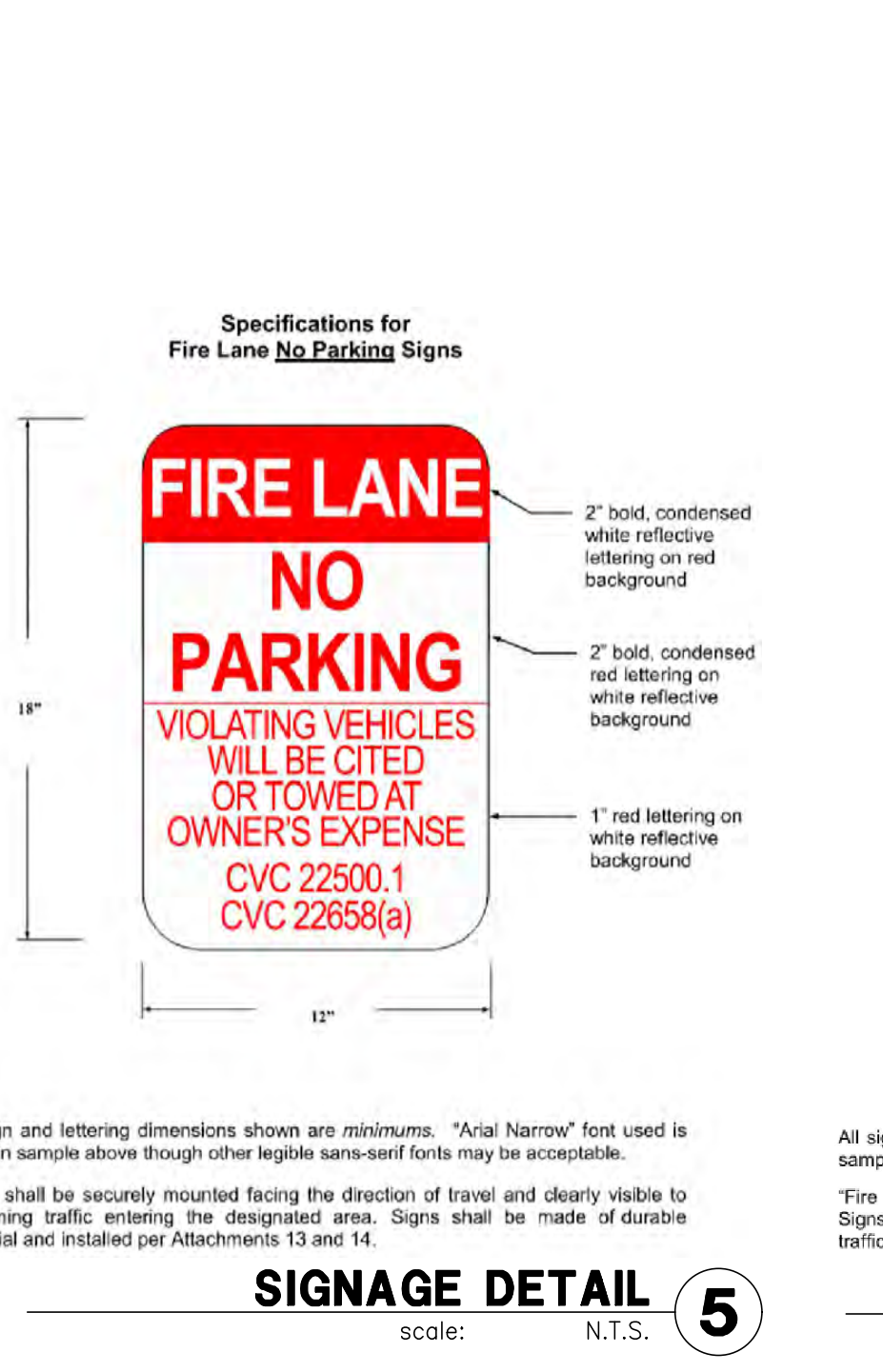
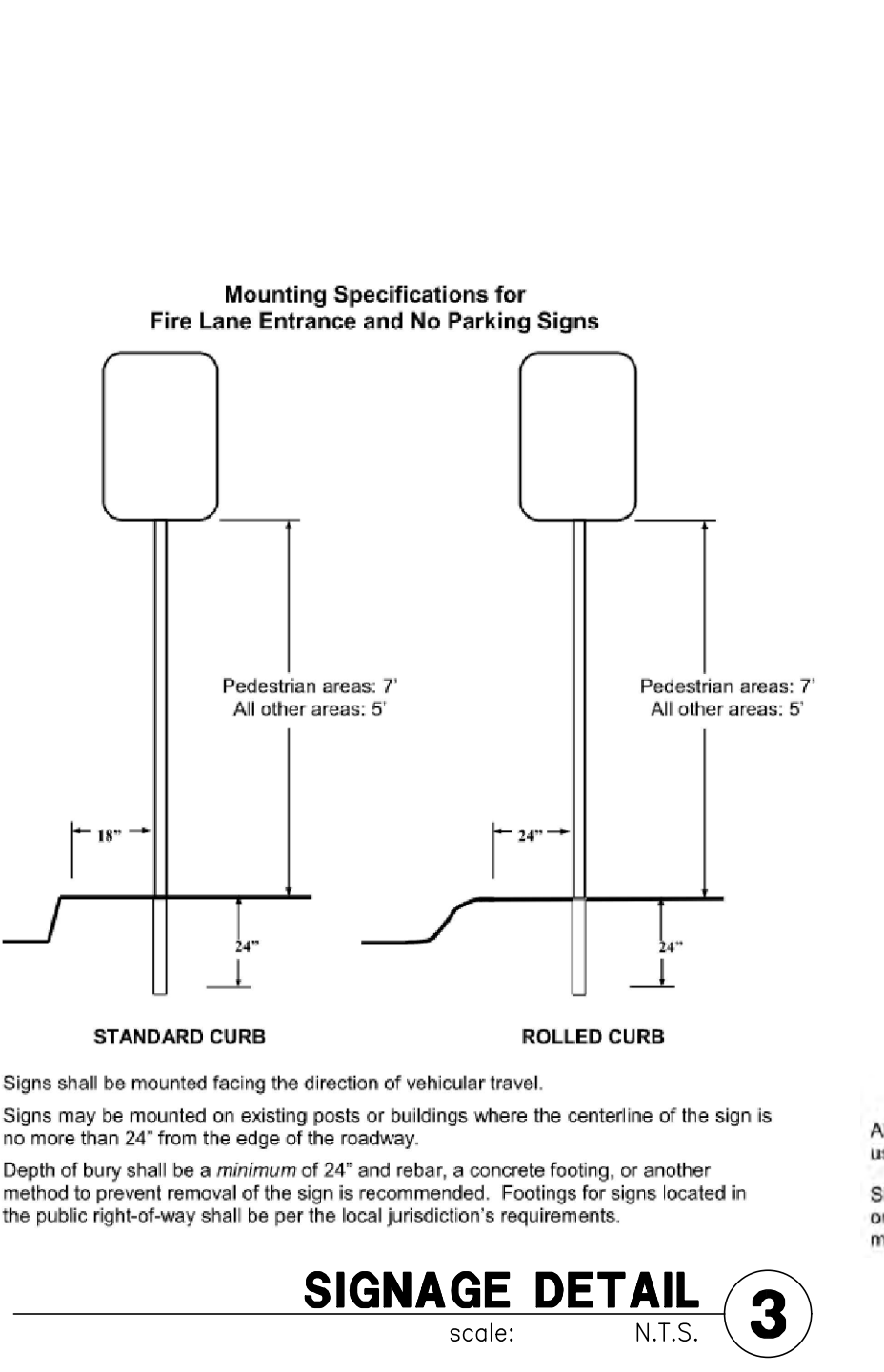
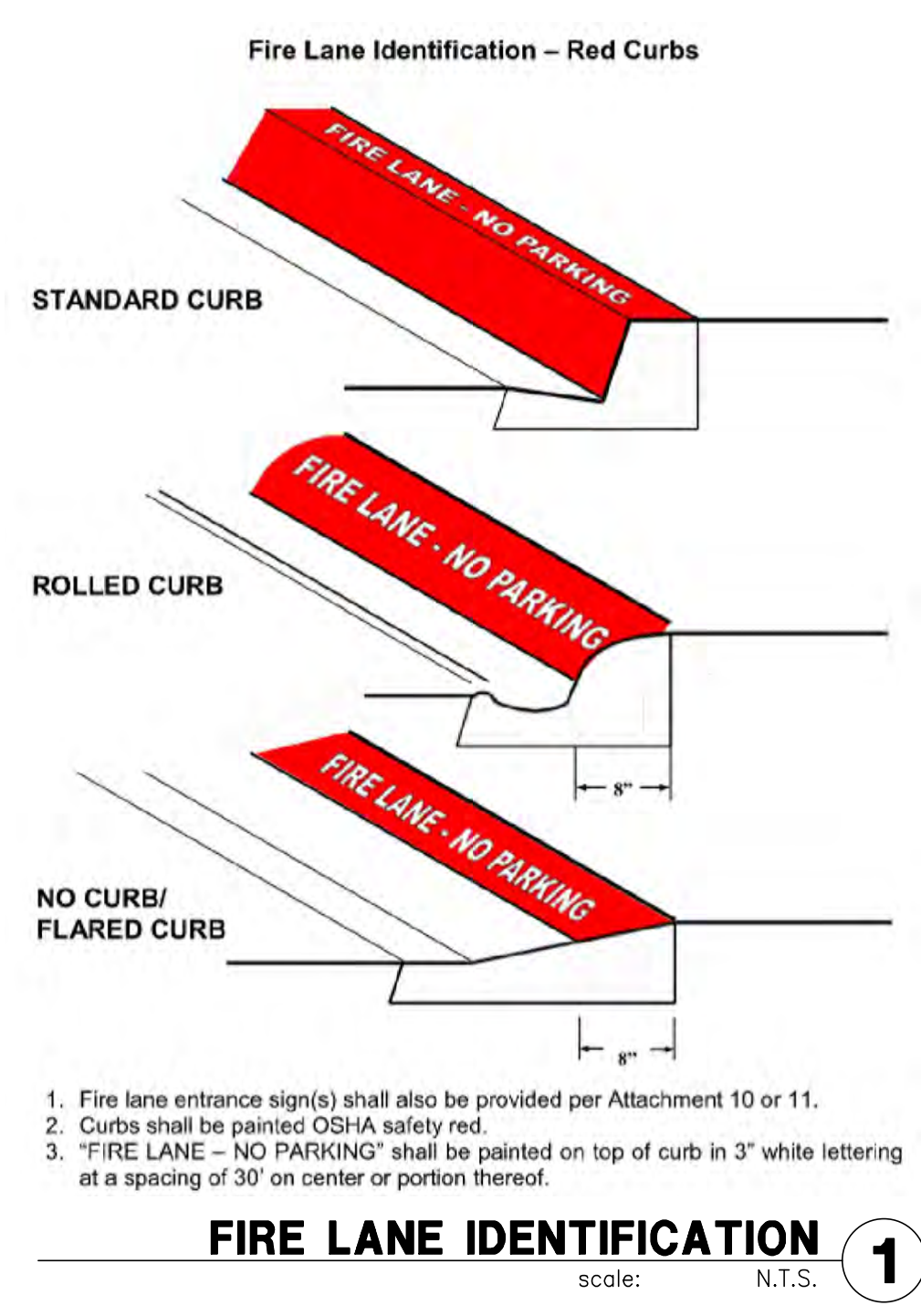
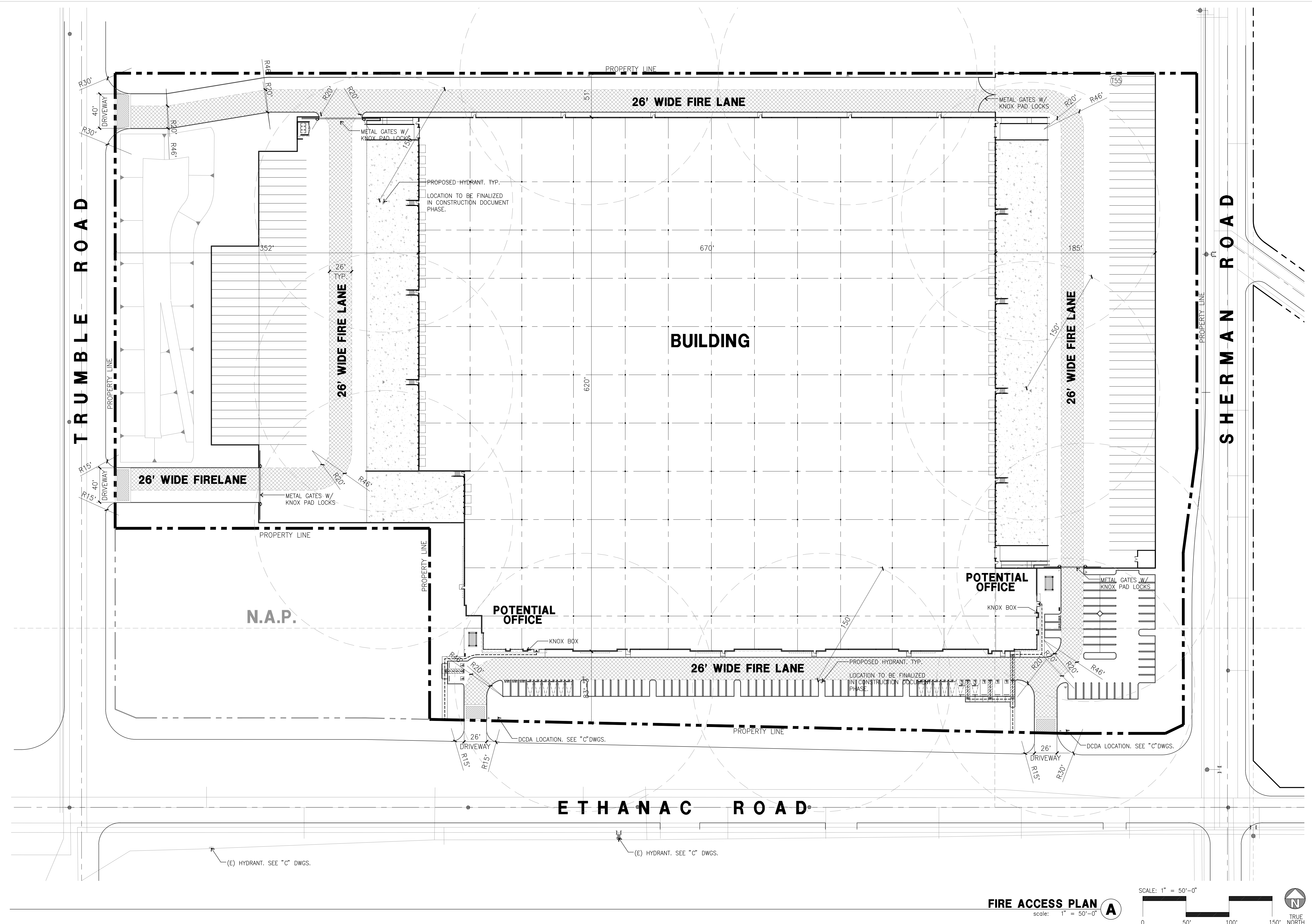
Title: **OVERALL SITE PLAN**

Project Number: 22215
Drawn by: KT
Date: 03/29/2023
Revision:
1ST SUBMITTAL 10/21/2022
2ND SUB / PC1 01/11/2023
3RD SUB / PC2 03/29/2023

Sheet:

DAB-A1.1

OFFICIAL USE ONLY



FIRE LANE NOTES
-75,000 LBS MINIMUM CAPACITY
-2.5% SLOPE MAX. SEE "C" DWGS

Towing company contact information is required for all properties with a standing written agreement for services with a towing company per the California Vehicle Code.
To facilitate periodic changes in towing company contracts, the towing company contact information may be posted on a separate sign mounted directly below the fire lane entrance sign instead of on the entrance sign itself. The method of attachment to the post shall not obscure the wording on either sign.



hpa, inc.
18831 bardeen avenue - ste. #100
irvine, ca
92612
tel: 949-863-1770
fax: 949-863-0851
email: hpa@hparchs.com

Owner:
HILLWOOD
A PEROT COMPANY

Hillwood
901 Via Piemonte, Ste 175
Ontario, CA 91764
909-256-5824
ATTN: John Grace

Project:
**NEC Ethanac
Road & Trumble
Road**

Perris, CA

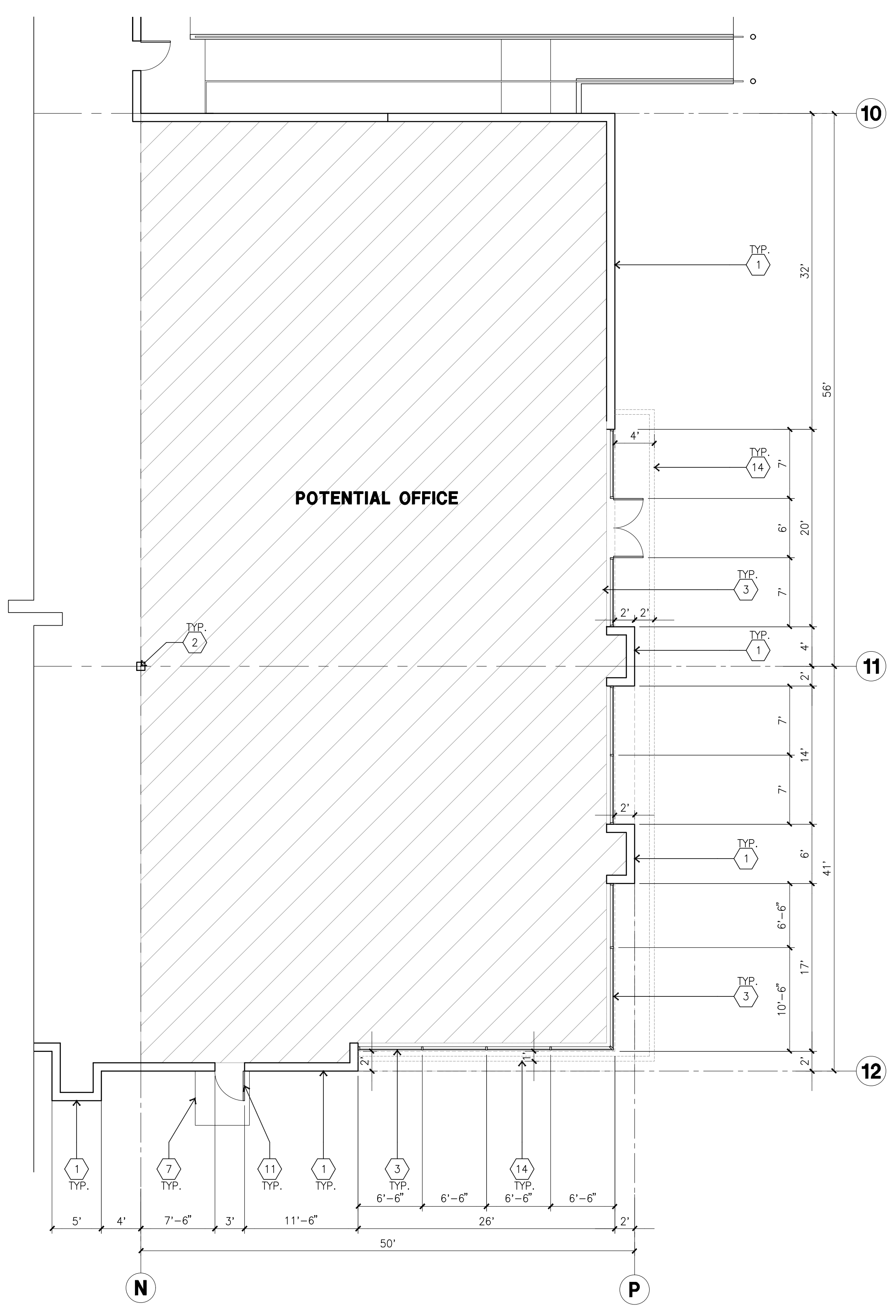
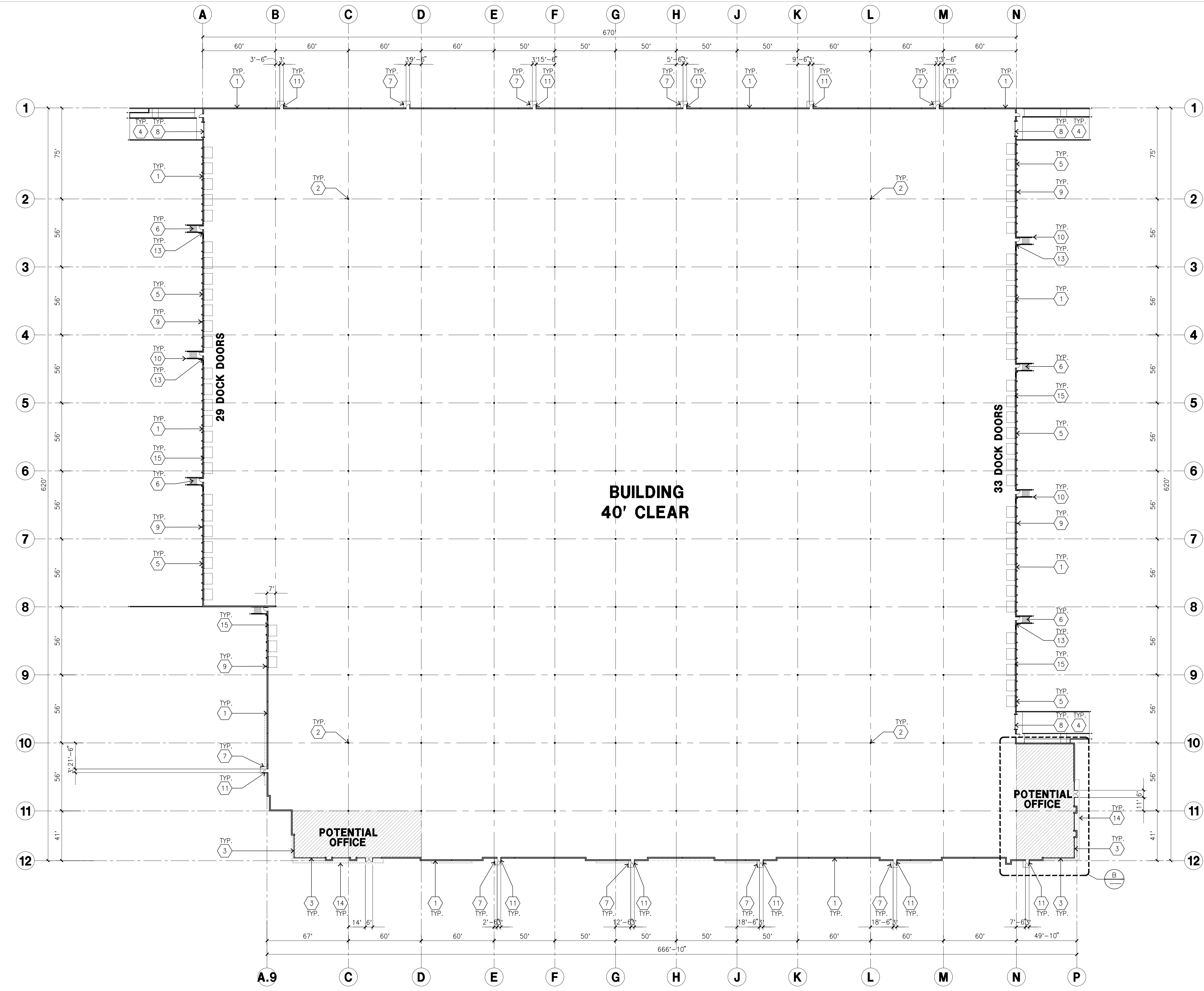
Consultants:
CIVIL - WEBB
STRUCTURAL -
MECHANICAL -
PLUMBING -
ELECTRICAL -
LANDSCAPE - WEBB
FIRE PROTECTION -
SOILS ENGINEER -

Title: **OVERALL FLOOR PLAN**

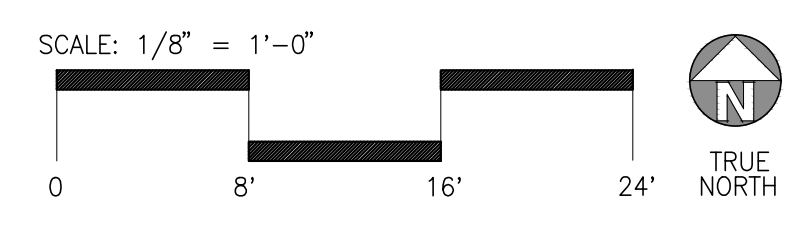
Project Number: 22215
Drawn by: KT
Date: 03/29/2023
Revision:
1ST SUBMITTAL 10/21/2022
2ND SUB / PC1 01/11/2023
3RD SUB / PC2 03/29/2023

Sheet:

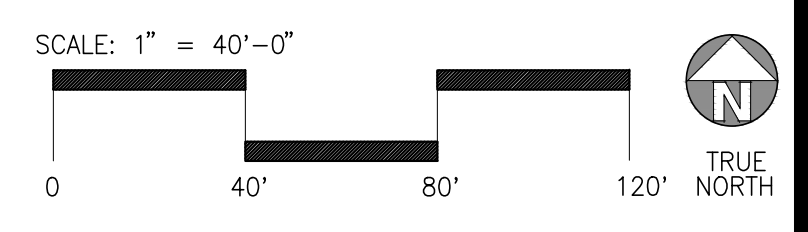
DAB-A2.1



ENLARGED FLOOR PLAN
scale: 1/8" = 1'-0" **B**



OVERALL FLOOR PLAN
scale: 1" = 40'-0" **A**

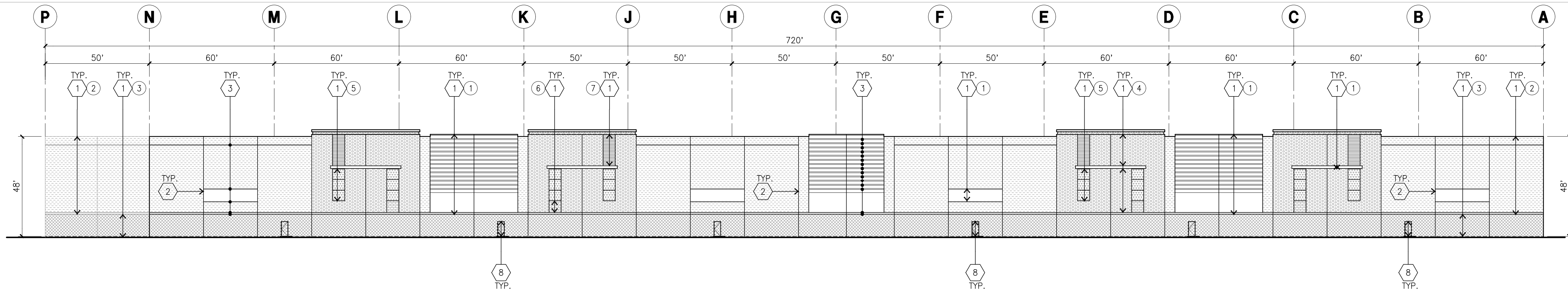


KEYNOTES - FLOOR PLAN

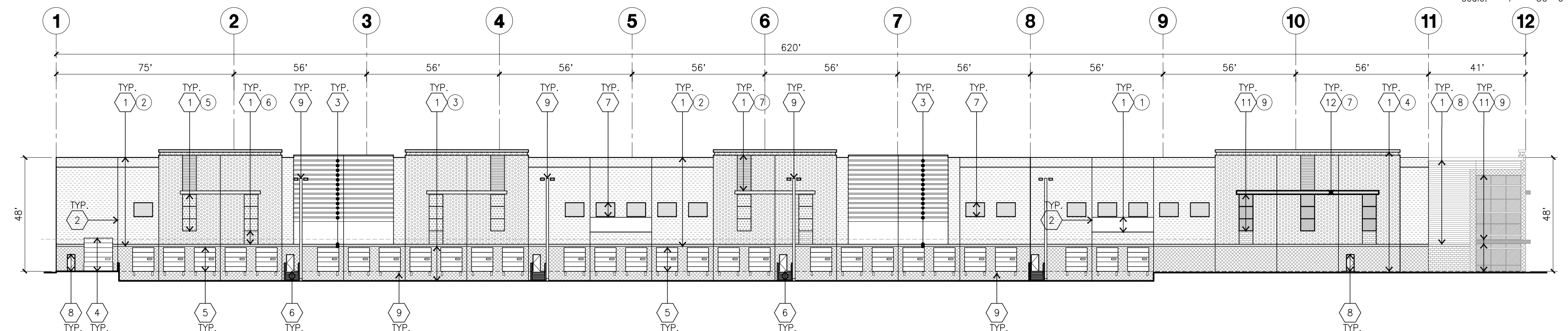
- 1 CONCRETE TILT-UP PANEL.
- 2 STRUCTURAL STEEL COLUMN.
- 3 TYPICAL STOREFRONT SYSTEM WITH GLAZING. SEE OFFICE BLOW-UP AND ELEVATIONS FOR SIZE, COLOR AND LOCATIONS.
- 4 CONCRETE RAMP W/ 42" HIGH CONC TILT-UP GUARD WALL OR BUILDING WALL ON BOTH SIDE OF RAMP.
- 5 9'-0" X 10' TRUCK DOOR, SECTIONAL O'H., STANDARD GRADE.
- 6 EXTERIOR CONCRETE STAIR
- 7 5'-6"x5'-6"x4" THICK CONCRETE EXTERIOR LANDING PAD TYPICAL AT ALL EXTERIOR MAN DOORS TO LANDSCAPED AREA. FINISH TO BE MEDIUM BLOOM FINISH. SLOPE TO BE 1/4" : 1' MAX. PROVIDE WALK TO HARD SURFACE PER CITY REQUIREMENTS.
- 8 12' X 14' DRIVE THRU, SECTIONAL O'H., STANDARD GRADE.
- 9 DOCK DOOR BUMPER
- 10 CONC. FILLED GUARD POST. 6" DIA. U.N.D., 42"H.
- 11 3X7" HOLLOW METAL EXTERIOR MAN DOOR.
- 12 SOFFIT LINE ABOVE
- 13 EXTERIOR DOWNSPOUT WITH OVERFLOW SCUPPER.
- 14 PAINTED I-BEAM CANOPY
- 15 Z GUARD
- 16 LONG TERM BICYCLE PARKING.

GENERAL NOTES - FLOOR PLAN

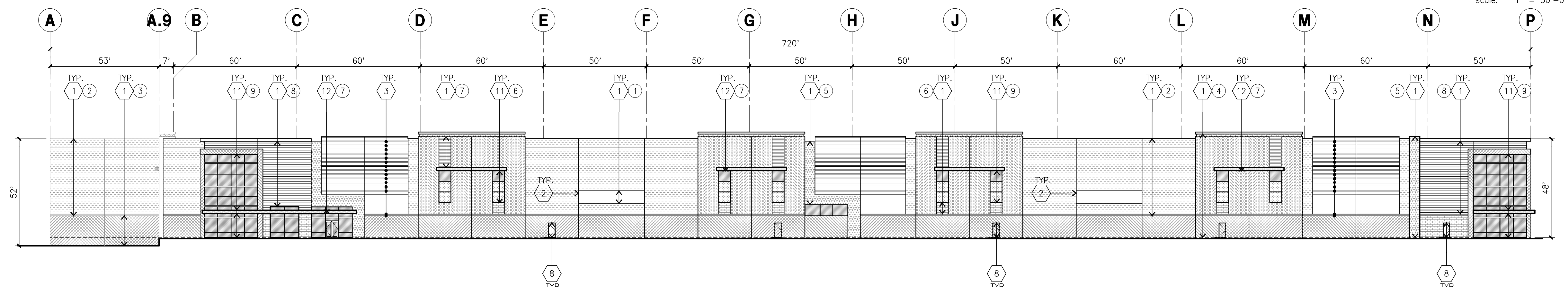
- A. THIS BUILDING IS DESIGNED FOR HIGH PILE STORAGE WITH FIRE ACCESS MAN DOORS AT 100' MAXIMUM O.C. A SEPARATE PERMIT WILL BE REQUIRED FOR ANY RACKING/CONVEYER SYSTEMS.
- B. FIRE HOSE LOCATIONS SHALL BE APPROVED PER FIRE DEPARTMENT.
- C. THE BUILDING FLOOR SLAB IS SLOPED, SEE "C" DRAWINGS FOR FINISH SURFACE ELEVATIONS.
- D. NOT USED
- E. WAREHOUSE INTERIOR CONCRETE WALLS ARE PAINTED WHITE. COLUMNS ARE TO RECEIVE PRIMER ONLY. ALL GYP. BD. WALLS IN WAREHOUSE TO RECEIVE 1 COAT OF WHITE TO CORNER.
- F. SLOPE POUR STRIP 1/2" TO EXTERIOR AT ALL MANDOR EXITS. SEE "S" DRAWINGS FOR POUR STRIP LOCATION.
- G. ALL DIMENSIONS ARE TO THE FACE OF CONCRETE PANEL WALL, GRIDLINE, OR FACE OF STUD U.N.D.
- H. SEE CIVIL DRAWINGS FOR POINT OF CONNECTIONS TO OFF-SITE UTILITIES. CONTRACTOR TO VERIFY ACTUAL UTILITY LOCATIONS.
- I. PLUMBING/ELECTRICAL COORDINATION.
- J. FOR DOOR TYPES AND SIZES. SEE DETAIL SHEET AD.4. NOTE: ALL DOORS PER DOOR SCHEDULE ARE FINISH OPENINGS.
- K. CONTRACTOR TO PROTECT AND KEEP THE FLOOR SLAB CLEAN. ALL EQUIPMENT TO BE DAMPERED INCLUDING CARS AND TRUCKS.
- L. ALL EXIT MAN DOORS IN WAREHOUSE TO HAVE ILLUMINATED EXIT SIGN. HARDWARE.
- M. HIGHLY FLAMMABLE AND COMBUSTIBLE MATERIAL SHALL NOT BE USED OR STORED IN THIS BUILDING.
- N. EACH EXTERIOR EXIT DOOR SHALL BE IDENTIFIED BY A TACTILE EXIT SIGN WITH THE WORDS "EXIT". THE MOUNTING HEIGHT FOR SUCH SIGNAGE SHALL BE 60" FROM FINISH FLOOR LEVEL TO THE CENTER OF THE SIGN.
- O. NON-ACCESSIBLE DOOR. PROVIDE WARNING SIGN LOCATED IN THE INTERIOR SIDE PER CBC 11338.1.1.1
- P. ALL ROOF MOUNTED MATERIALS SHALL BE FULLY SCREENED FROM PUBLIC VIEW, SEE A/44-1 OFFICE SECTION.



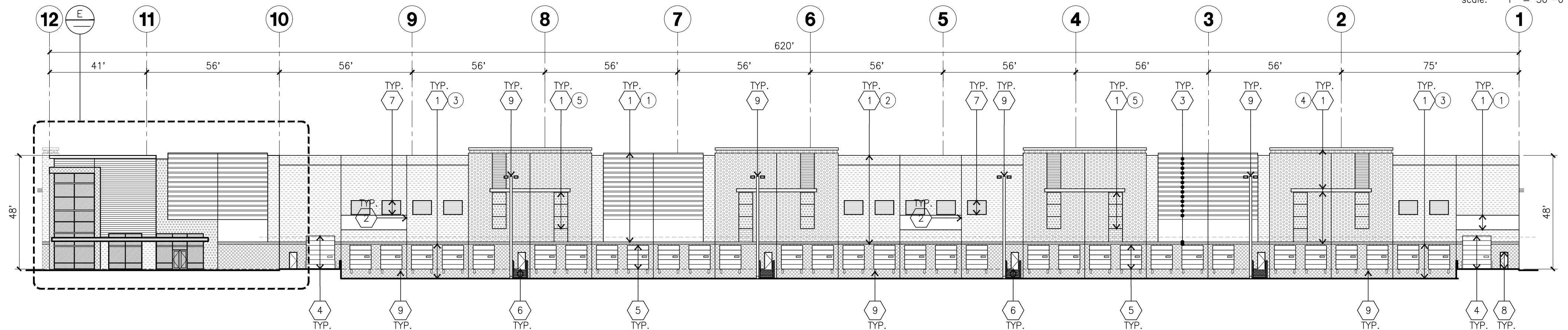
NORTH ELEVATION A
scale: 1/8" = 1'-0"



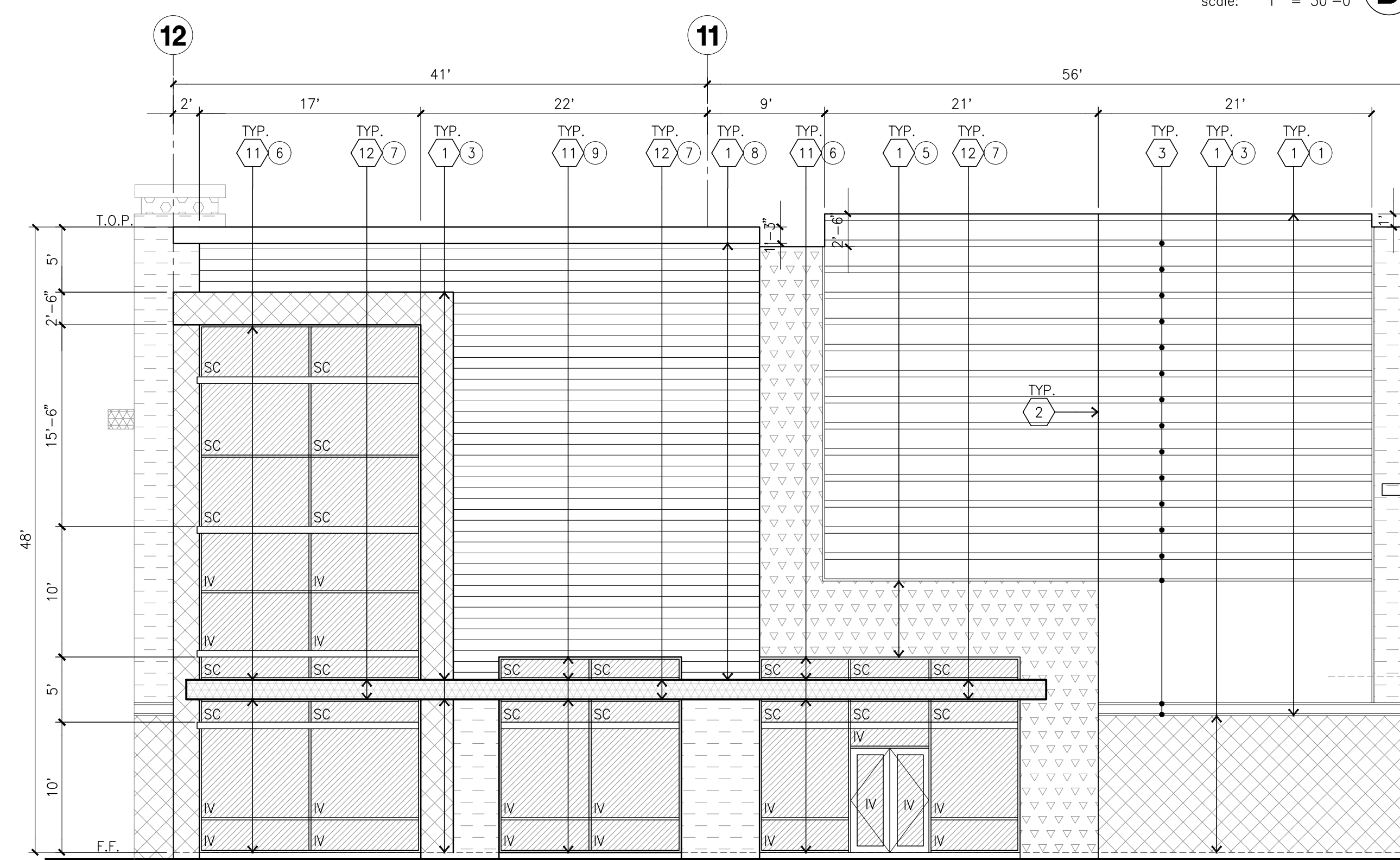
WEST ELEVATION B
scale: 1/8" = 1'-0"



SOUTH ELEVATION C
scale: 1/8" = 1'-0"



EAST ELEVATION D
scale: 1/8" = 1'-0"



ENLARGED EAST ELEVATION E
scale: 1/8" = 1'-0"

KEYNOTES - ELEVATIONS

- 1 CONCRETE TILT-UP PANEL (PAINTED). FINISH GRADE VARIES. SEE "C" DRAWINGS. WATERPROOF ALL WALLS WHERE GRADE IS HIGHER AND EXPOSED TO THE WEATHER ONE SIDE. WATERPROOFING TO BE PROTECTED WITH PROTECTION BOARD AND A MIN. OF 6" OF GRAVEL. PROVIDE TRENCH DRAIN AT BOTTOM AND DRAINAGE TO CURB OR TAKE TO STORM DRAIN. NOT REQUIRED AT DOCK HIGH CONDITION OR AT RAMP WALLS.
- 2 PANEL JOINT.
- 3 PANEL REVEAL. ALL REVEALS TO HAVE A MAX. OF 3/8" CHAMFER. REVEAL COLOR TO MATCH ADJACENT BUILDING FIELD COLOR. U.N.O.
- 4 OVERHANG DOOR @ DRIVE THRU. SEE DOOR SCHEDULE. PROVIDE COMPLETE WEATHER-STRIPPING PROTECTION ALL AROUND.
- 5 OVERHANG DOOR @ DOCK HIGH. SEE DOOR SCHEDULE. PROVIDE COMPLETE WEATHER-STRIPPING PROTECTION ALL AROUND.
- 6 CONCRETE STAIR, LANDING AND GUARDRAIL W/ METAL PIPE HANDRAIL. PROVIDE NON SKID NOSING TO MEET ADA REQUIREMENTS. PROVIDE CONTRASTING COLORED 3" WIDE WARNING STRIPE INTEGRAL TO CONCRETE AT TOP LANDING AND BOTTOM TREAD PER ADA REQUIREMENTS.
- 7 METAL LOUVER. PAINT TO MATCH BUILDING COLOR.
- 8 3X7" HOLLOW METAL EXTERIOR MAN DOOR. PROVIDE COMPLETE WEATHER STRIPING ALL AROUND DOOR. PROVIDE FOR RAIN DIVERTER ABOVE DOOR.
- 9 EXTERIOR DOWNSPOUT AND OVERFLOW SCUPPER
- 10 DOCK BUMPER
- 11 ALUMINUM STOREFRONT FRAMING WITH TEMPERED GLAZING AT ALL DOORS, SIDELITES ADJACENT TO DOORS AND GLAZING WITH BOTTOMS LESS THAN 18" ABOVE FINISH FLOOR ELEVATION.
- 12 PAINTED I-BEAM CANOPY

GENERAL NOTES - ELEVATIONS

- A. ALL PAINT COLOR CHANGES TO OCCUR AT INSIDE CORNERS UNLESS NOTED OTHERWISE.
- B. ALL PAINT FINISHES ARE TO BE FLAT UNLESS NOTED OTHERWISE.
- C. T.O.P. EL. = TOP OF PARAPET ELEVATION.
- D. F.F. = FINISH FLOOR ELEVATION.
- E. STOREFRONT CONSTRUCTION: GLASS, METAL ATTACHMENTS AND LINTELS SHALL BE DESIGNED TO RESIST 90 MPH EXPOSURE "C" WINDS. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS PRIOR TO INSTALLATION.
- F. CONTRACTOR SHALL FULLY PAINT ONE CONCRETE PANEL W/ SELECTED COLORS ARCHITECT AND OWNER SHALL APPROVE PRIOR TO PAINTING REMAINDER OF BUILDING.
- G. BACK SIDE OF PARAPETS TO HAVE SMOOTH FINISH AND BE PAINTED WITH ELASTOMERIC PAINT.
- H. FOR SPANDREL GLAZING, ALLOW SPACE BEHIND SPANDREL TO BREATHE. J. USE ADHESIVE BACK WOOD STRIPS FOR ALL REVEAL FORMS.
- K. THE FIRST COAT OF PAINT TO BE ROLLED-ON AND THE SECOND COAT TO BE SPRAYED-ON

COLOR SCHEDULE - ELEVATIONS

1	CONCRETE TILT-UP PANEL	PAINT BRAND, SHERWIN WILLIAMS SW 7005 (255-C1) PURE WHITE
2	CONCRETE TILT-UP PANEL	PAINT BRAND, SHERWIN WILLIAMS SW 7071 (235-C1) GRAY SCREEN
3	CONCRETE TILT-UP PANEL	PAINT BRAND, SHERWIN WILLIAMS SW 7073 (235-C3) NETWORK GRAY
4	CONCRETE TILT-UP PANEL	PAINT BRAND, SHERWIN WILLIAMS SW 7074 (235-C5) SOFTWARE
5	CONCRETE TILT-UP PANEL	PAINT BRAND, SHERWIN WILLIAMS SW 6524 (185-C7) COMMODORE
6	CORONADO STONE	PAINT BRAND, WIRECUT BRICK - 2 1/2" x 8" COLOR: ICEBERG. GROUT: WHITE
7	METAL PANEL CLADDING	PAINT BRAND, BRIDGESTEEL SHIPLAP WALL - 12" ELASTOMERIC PAINT
8	METAL PANEL CLADDING	PAINT BRAND, BRIDGESTEEL SHIPLAP WALL - 12" COLOR: SLATE GRAY
9	GLAZING/MULLIONS	PAINT BRAND, CLEAR GLAZING WITH CLEAR ANODIZED ALUMINUM MULLIONS
10	METAL CANOPY	PAINT BRAND, SHERWIN WILLIAMS SW 7005 (255-C1) PURE WHITE

GLAZING LEGEND

- ALL GLAZING TO BE TEMPERED GLASS.
- IV : INSULATED VISION GLASS
 - SC : SPANDREL GLASS WITH CONCRETE BEHIND
 - SL : SINGLE LITE VISION GLASS
- IV : INSULATED VISION GLASS
1/4" VISTACOL PACIFICA + 1/4" SOLARBAN 60 CLEAR
1" INSULATED GLASS UNIT WITH 1/2" AIRSPACE AND 1/4" LITES
U: 0.27 SHGC: 0.21 VLT 28%
MINIMUM VT TO BE 0.42 PER 2016 DEC TABLE 140.3-B
- SC : SPANDREL WITH CONCRETE BEHIND
1/4" VISTACOL PACIFICA WITH WARM GRAY OPACICOAT PAINTED ON REFLECTIVE.
INSTALLED ON CONCRETE.
- V : VISION GLASS
1/4" VISTACOL PACIFICA
- MULLIONS : ANODIZED CLEAR.



hpa, inc.
18831 bardeen avenue, - ste. #100
irvine, ca
92612
tel: 949-863-1770
fax: 949-863-0851
email: hpa@hparchs.com

Owner:
HILLWOOD
A PEROT COMPANY

Hillwood
901 Via Pilemonte, Ste 175
Ontario, CA 91764
909-256-5824
ATTN: John Grace

Project:
**NEC Ethanac
Road & Trumble
Road**

Perris, CA

Consultants:

CIVIL	WEBB
STRUCTURAL	-
MECHANICAL	-
PLUMBING	-
ELECTRICAL	-
LANDSCAPE	WEBB
FIRE PROTECTION	-
SOILS ENGINEER	-

Title: ELEVATIONS

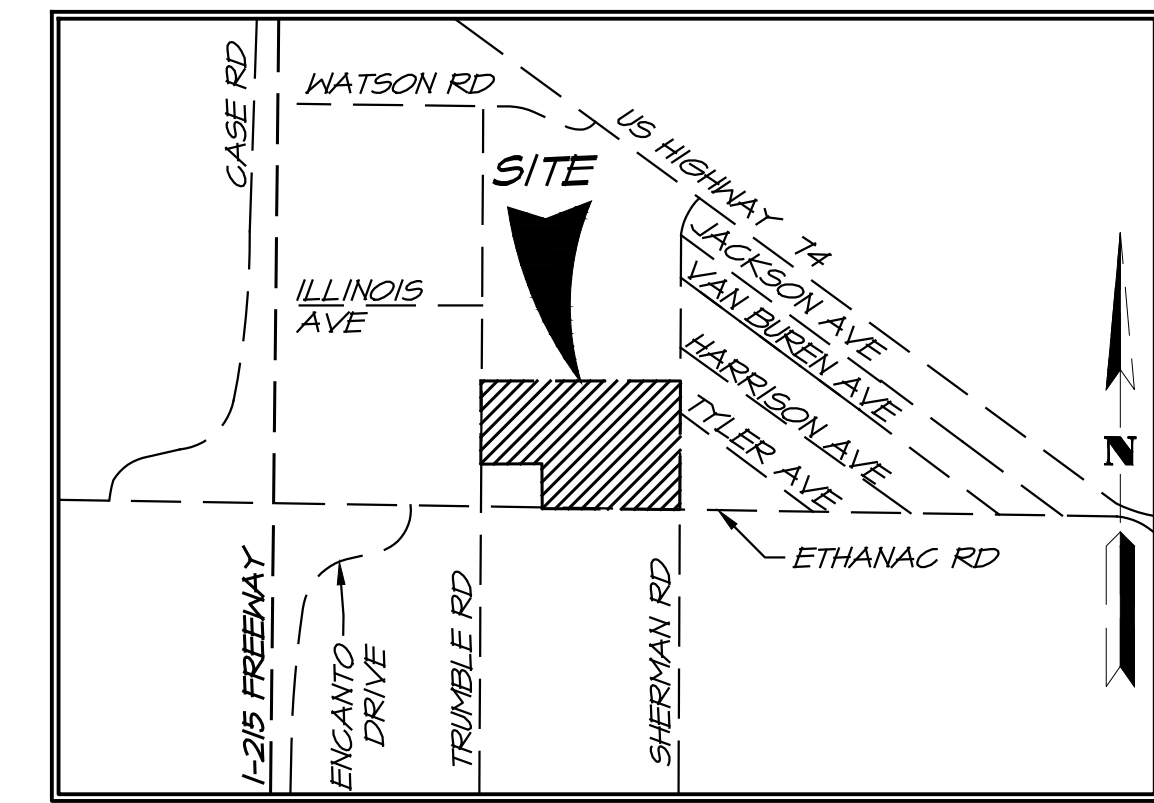
Project Number: 22215
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3RD SUB / PC2 03/29/2023

Sheet:

DAB-A3.1

IN THE CITY OF PERRIS, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA DEVELOPMENT PLAN REVIEW NO. 22-00030

LOCATED IN SECTION 10 SW, T. 5S., R. 3W., S.B.M.



VICINITY MAP

OWNER/APPLICANT
HILLWOOD INVESTMENT PROPERTIES
901 VIA PIEMONTE STE. 175
ONTARIO CA, 91764
CONTACT: JOHN GRACE
EMAIL: JOHN.GRACE@HILLWOOD.COM
PHONE: (909) 256-5424

PROJECT REPRESENTATIVE
ALBERT A. WEBB ASSOCIATES
3788 MCCRAY STREET
RIVERSIDE, CA 92506
CONTACT: RICHARD BELMUEZ
PHONE: (951) 686-1070
FAX: (951) 788-1256

SOILS ENGINEER
SOUTHERN CALIFORNIA GEOTECHNICAL
22885 E. SAVI RANCH PARKWAY,
SUITE E
YORBA LINDA, CA 92887
CONTACT: ROBERT TRAZO
PHONE: (714) 685-1115
FAX: (714) 685-1118

ENGINEER
ALBERT A. WEBB ASSOCIATES
3788 MCCRAY STREET
RIVERSIDE, CA 92506
CONTACT: SARAH KOWALSKI
PHONE: (951) 686-1070
FAX: (951) 788-1256

A.P.N.
324-240-016 THRU -020, -023,
THRU -027

ACREAGE
GROSS 21.6 AC
NET 19.9 AC
R/W 1.7 AC

ARCHITECT
HPA ARCHITECTS
18831 BARDEEN AVE., STE 100
IRVINE, CA 92612
CONTACT: INKON KIM
PHONE: (949) 863-1170
EMAIL: INKON@HPARCHS.COM

TOPOGRAPHY SOURCE
TOPOGRAPHY FLOWN BY INLAND AERIAL
SURVEYS INC. ON 02/15/2022

LAND USE
EXISTING LAND USE: VACANT
EXISTING GENERAL PLAN LAND USE: COMMERCIAL COMMUNITY (CC)
EXISTING ZONING: COMMERCIAL COMMUNITY (CC)
PROPOSED GENERAL PLAN LAND USE: LIGHT INDUSTRIAL (LI)
PROPOSED ZONING: LIGHT INDUSTRIAL (LI)

PROJECT DATA

BUILDING AREA	10,000 SF
OFFICE - FIRST FLOOR	5,000 SF
OFFICE - SECOND FLOOR	5,000 SF
WAREHOUSE	347,348 SF
TOTAL BUILDING FOOTPRINT	407,348 SF
TOTAL FLOOR AREA	412,348 SF

AUTO PARKING REQUIRED

OFFICE:	N/A
(1 STALL/300 SF IF LESS THAN 10% GFA)	
WAREHOUSE:	105 STALLS
400K SF @ (30 STALLS + 1 STALL/5,000 SF)	
TOTAL:	105 STALLS

AUTO PARKING PROVIDED

STANDARD (9' x 19')	83 STALLS
ADA STANDARD (9' x 19')	3 STALLS
ADA VAN ACCESSIBLE (12' x 19')	2 STALLS
EV STANDARD (9' x 19')	11 STALLS
EV ADA STANDARD (9' x 19')	1 STALL
EV ADA VAN ACCESSIBLE (12' x 19')	1 STALL
CLEAN AIR/VANPOOL/EV (9' x 19')	5 STALLS
TOTAL:	106 STALLS

TRAILER PARKING REQUIRED
175,000 SF 82 STALLS

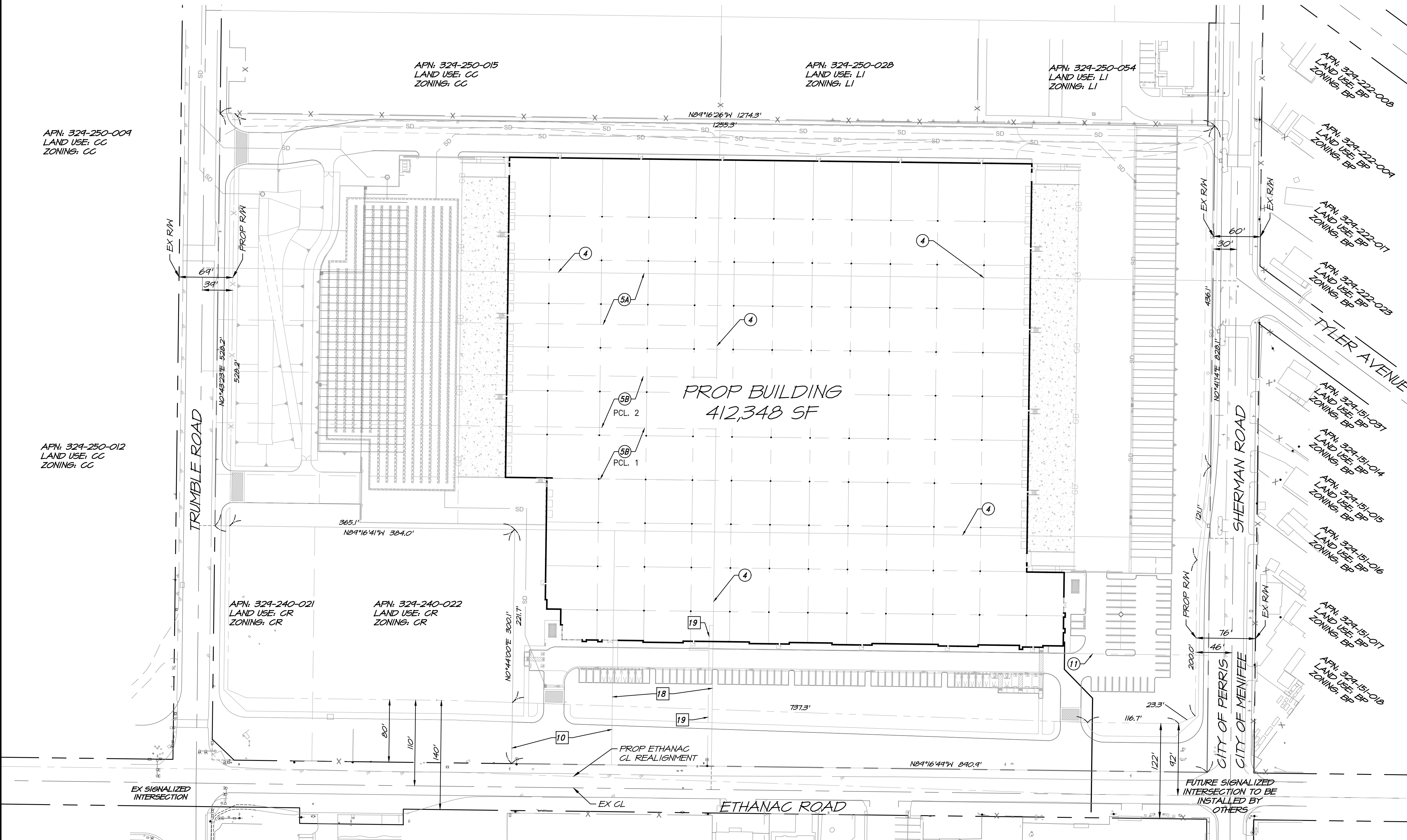
TRAILER PARKING PROVIDED
TYPICAL TRAILER (10' x 55') 112 STALLS

EARTHWORK ESTIMATE

CUT:	21,600 CY
FILL:	62,200 CY
NET:	34,800 CY (FILL)

SHEET INDEX

SHEET 1 - TITLE SHEET	
SHEET 2 - STREET AND GRADING SECTIONS	
SHEET 3 - CONCEPTUAL GRADING	
SHEETS 4,5 - CONCEPTUAL UTILITIES	



GENERAL INFORMATION

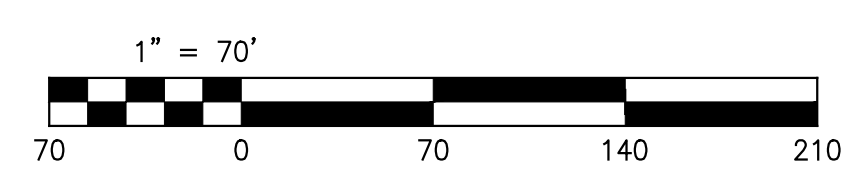
- ALL PARCELS WITHIN PROJECT BOUNDARY TO BE COMBINED VIA PARCEL MAP
- THOMAS BROS. MAP BOOK PAGE: 038, GRID: C1 AND D1
- PROJECT IS NOT WITHIN A SPECIFIC PLAN
- PROJECT LIES WITHIN THE CITY OF PERRIS REDEVELOPMENT PROJECT AREA.
- PROJECT LIES WITHIN CFD NO. 1.
- EASEMENTS OF RECORD ARE PLOTTED HEREIN.
- PROJECT IS WITHIN EASTERN MUNICIPAL WATER DISTRICT
- THERE ARE NO EXISTING WELLS ON THE PROPERTY.
- ALL SLOPES ARE 2:1 RATIO, UNLESS OTHERWISE NOTED.
- LAND IS NOT WITHIN A SPECIAL STUDIES ZONE.
- LAND HAS LOW POTENTIAL FOR LIQUEFACTION PER SOCAL GEO REPORT DATED 02/23/2022.
- SUB-SURFACE SEPTIC DISPOSAL IS NOT INTENDED ON SITE.
- STRUCTURES AND/OR DWELLINGS DO NOT EXIST ON SITE.
- THE PROJECT WILL COMPLY WITH NPDES REQUIREMENTS AS REQUIRED BY NPDES SUPPLEMENT "A".
- FLOOD ZONE X, AREA OF LOW FLOODING PER FEMA PANEL 06065C2060H.
- PROJECT LIES WITHIN AIRPORT LAND USE COMPATIBILITY ZONE D
- ARCHITECTURAL SITE PLAN PROVIDED BY HPA ARCHITECTURE ON 04/12/23

UTILITY PROVIDERS

WATER:	EASTERN MUNICIPAL WATER DISTRICT PHONE: (951) 685-7434
SEWER:	EASTERN MUNICIPAL WATER DISTRICT PHONE: (951) 685-7434
GAS:	SOUTHERN CALIFORNIA GAS COMPANY PHONE: (909) 307-1070
ELECTRIC:	SOUTHERN CALIFORNIA EDISON PHONE: 1 (800) 655-4555
TELEPHONE:	CHARTER COMMUNICATIONS (SPECTRUM) PHONE: (951) 406-1666
CABLE T.V.:	FRONTIER COMMUNICATIONS PHONE: (310) 264-5100
SCHOOL DISTRICT(S):	ROMOLAND SCHOOL DISTRICT (TK-8) PHONE: (951) 426-4244 PERRIS UNION HIGH SCHOOL DISTRICT (9-12) PHONE: (951) 943-6369

EASEMENT NOTES
SEE SHEET 2

LEGAL DESCRIPTION
SEE SHEET 2



PUBLIC DETENTION BASIN ADDED	4/23	AG
REVISIONS	DATE	BY

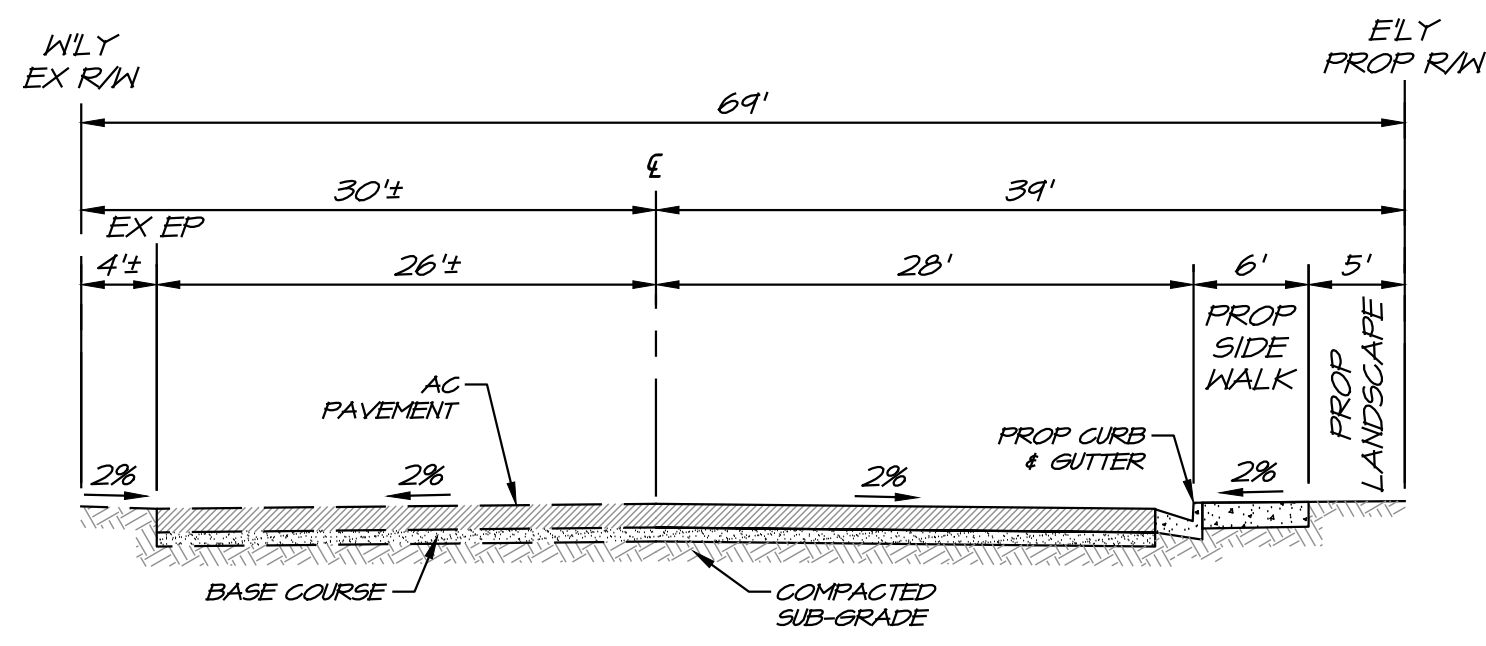
CITY OF PERRIS
ETHANAC COMMERCE CENTER
TITLE SHEET
DEVELOPMENT PLAN REVIEW NO. 22-00030

SCALE: 1" = 70'	DATE: 4/17/2023	DESIGNED: AG	CHECKED: SK	PLN CK REF: F.B.
W.O. 22-0198	SHEET 1	OF 5 SHEETS	DWG. NO.	

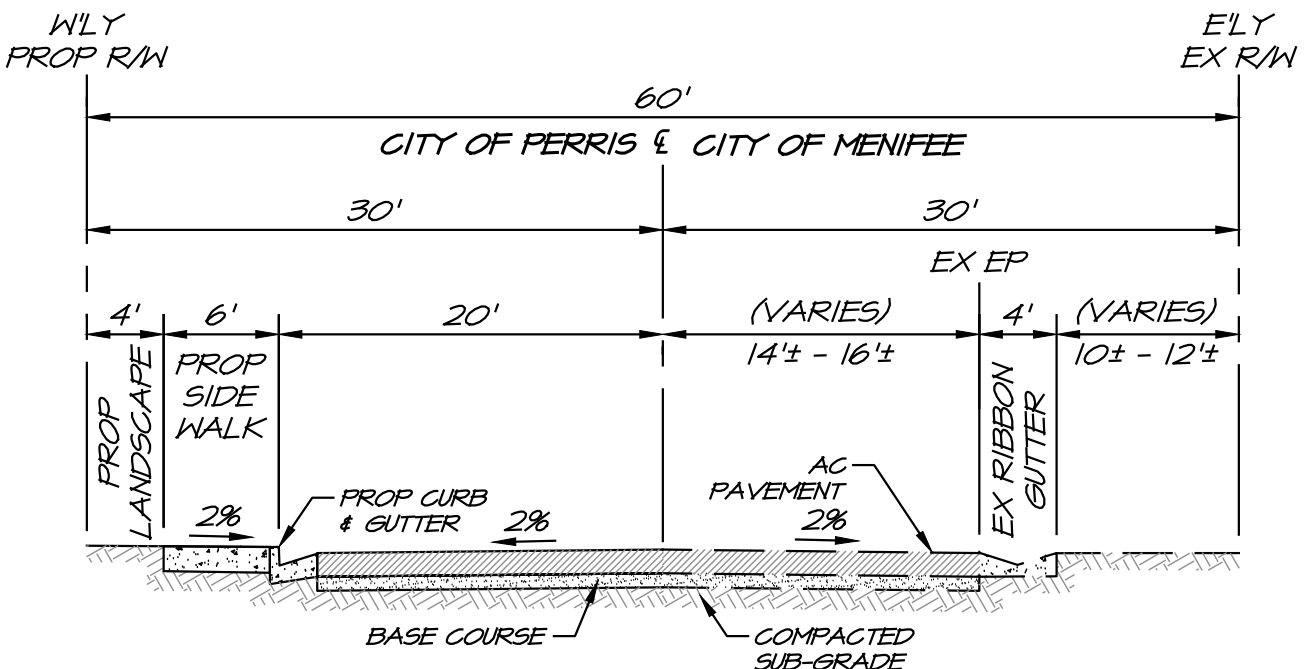
ALBERT A. WEBB ASSOCIATES
ENGINEERING CONSULTANTS
3788 MCCRAY STREET
RIVERSIDE, CA 92506
PH. (951) 686-1070
FAX (951) 788-1256

PRELIMINARY

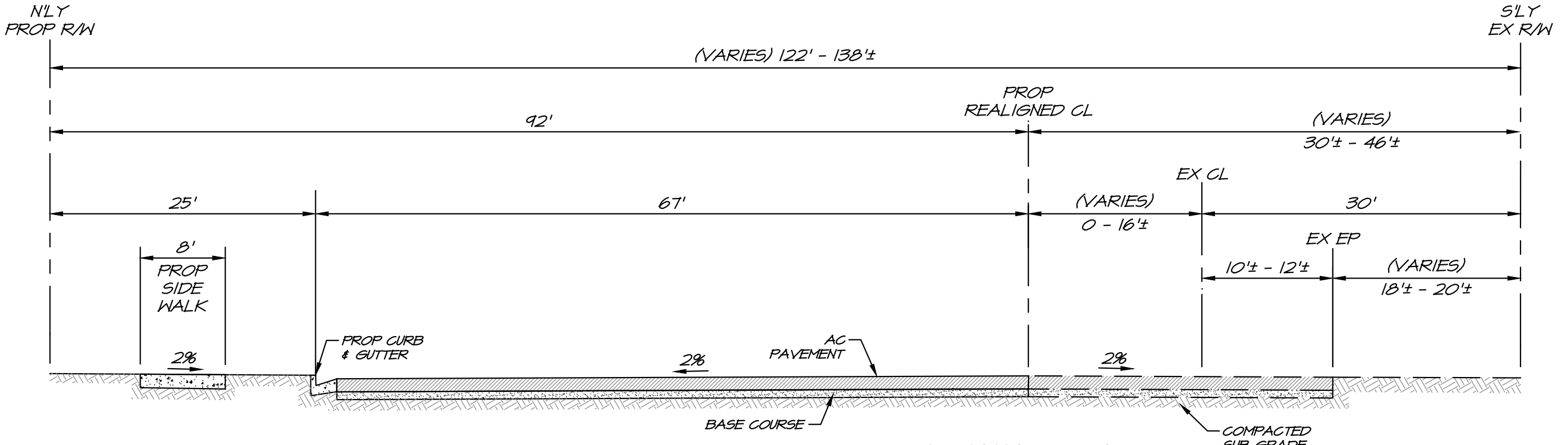
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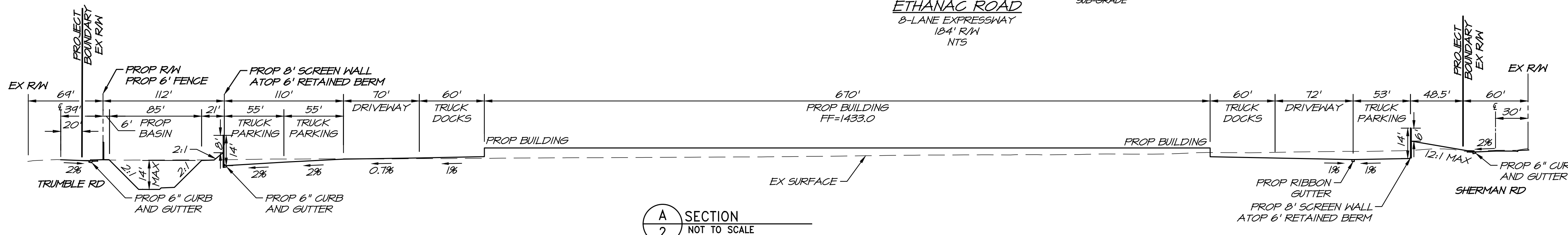
TRUMBLE ROAD
MAJOR COLLECTOR
18' R/W
NTS



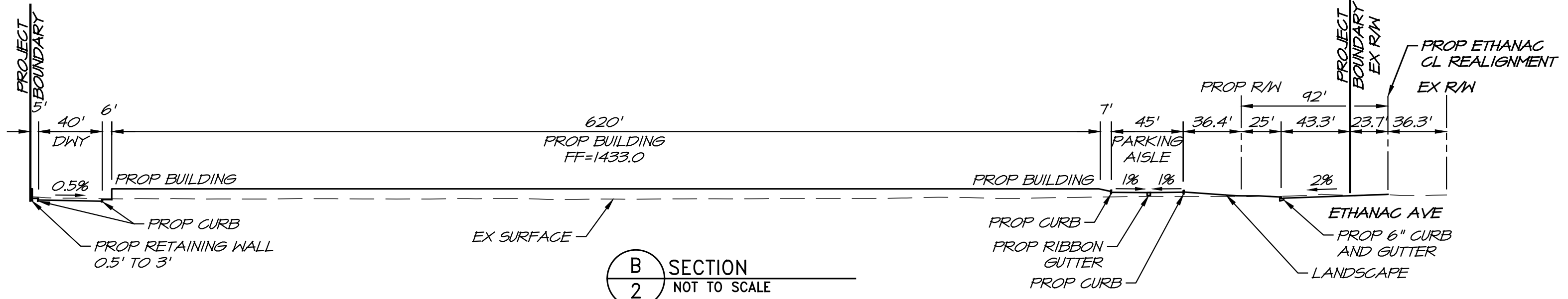
SHERMAN ROAD
LOCAL
60' R/W
NTS



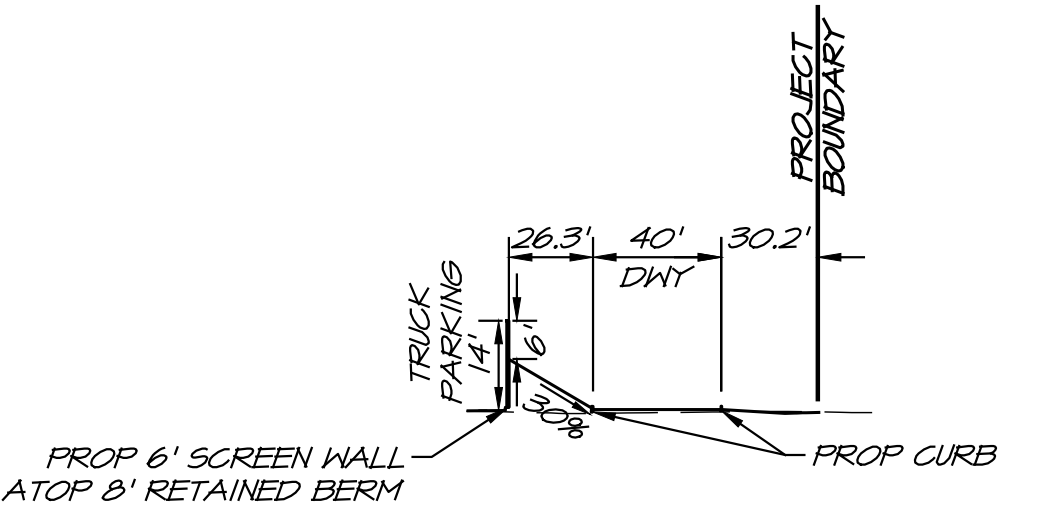
ETHANAC ROAD
8-LANE EXPRESSWAY
18' R/W
NTS



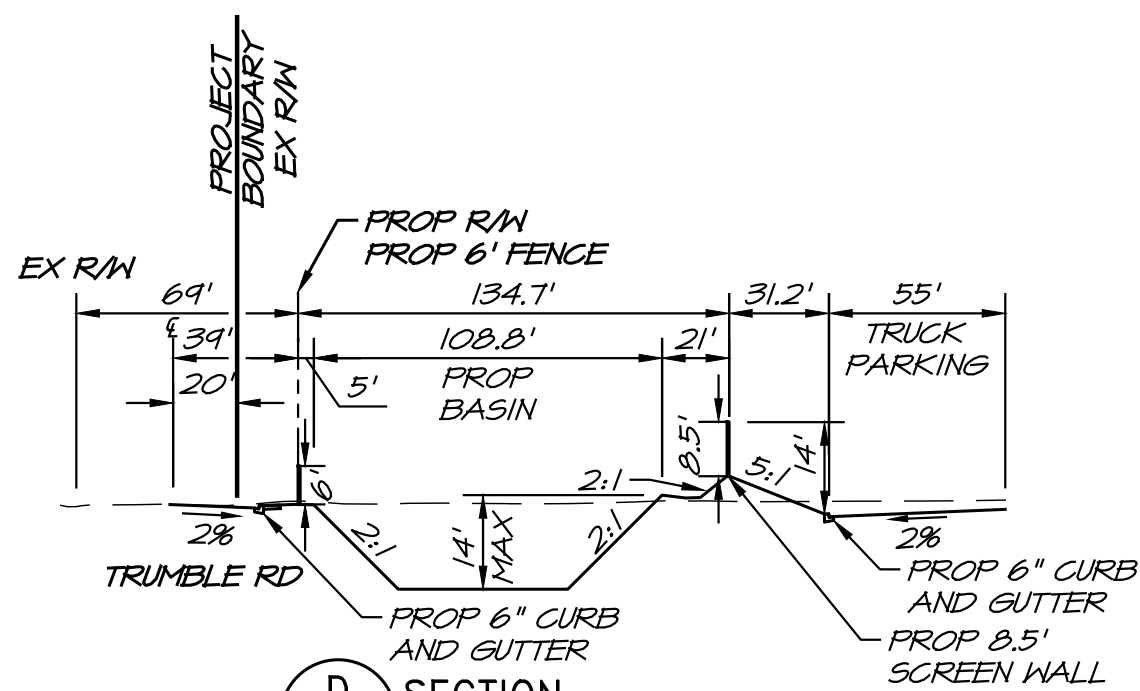
A SECTION
2
NOT TO SCALE



B SECTION
2
NOT TO SCALE



C SECTION
2
NOT TO SCALE



D SECTION
2
NOT TO SCALE

EASEMENT NOTES

- APN: 329-240-016 THRU -020 AND 329-240-025 THRU -027.
- 4. AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED JUNE 25, 1921 IN BOOK 120 OF DEEDS, PAGE 209, IN FAVOR OF SOUTHERN SIERRAS POWER COMPANY. THE LOCATION OF THE EASEMENT CANNOT BE DETERMINED FROM RECORD INFORMATION.
 - 5. COVENANTS, CONDITIONS, RESTRICTIONS AND EASEMENTS IN THE DOCUMENT RECORDED APRIL 17, 1934 AS BOOK 170, PAGE 305 AND JANUARY 13, 1936 IN BOOK 259, PAGE 560 (BOTH) OF OFFICIAL RECORDS, BUT DELETING ANY COVENANT, CONDITION OR RESTRICTION INDICATING A PREFERENCE, LIMITATION OR DISCRIMINATION BASED ON RACE, COLOR, RELIGION, SEX, HANDICAP, FAMILIAL STATUS, NATIONAL ORIGIN, SEXUAL ORIENTATION, MARITAL STATUS, ANCESTRY, SOURCE OF INCOME OR DISABILITY, TO THE EXTENT SUCH COVENANTS, CONDITIONS OR RESTRICTIONS VIOLATE TITLE 42, SECTION 3604(C), OF THE UNITED STATES CODES. LAWFUL RESTRICTIONS UNDER STATE AND FEDERAL LAW ON THE AGE OF OCCUPANTS IN SENIOR HOUSING OR HOUSING FOR OLDER PERSONS SHALL NOT BE CONSTRUED AS RESTRICTIONS BASED ON FAMILIAL STATUS.
[5A] BOOK 170, PAGE 305 & [5B] BOOK 259, PAGE 560]
 - 11. AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED AUGUST 8, 1947 AS BOOK 846, PAGE 355 OF OFFICIAL RECORDS, IN FAVOR OF CALIFORNIA ELECTRIC POWER COMPANY.
[& OF UNDISCLOSED WIDTH]

APN: 329-240-023 AND -024

- 10. COVENANTS, CONDITIONS, RESTRICTIONS AND EASEMENTS IN THE DOCUMENT RECORDED APRIL 24, 1933 AS BOOK 120, PAGE 397 OF OFFICIAL RECORDS, WHICH PROVIDE THAT A VIOLATION THEREOF SHALL NOT DEFEAT OR RENDER INVALID THE LIEN OF ANY FIRST MORTGAGE OR DEED OF TRUST MADE IN GOOD FAITH AND FOR VALUE, BUT DELETING ANY COVENANT, CONDITION OR RESTRICTION INDICATING A PREFERENCE, LIMITATION OR DISCRIMINATION BASED ON RACE, COLOR, RELIGION, SEX, HANDICAP, FAMILIAL STATUS, NATIONAL ORIGIN, SEXUAL ORIENTATION, MARITAL STATUS, ANCESTRY, SOURCE OF INCOME OR DISABILITY, TO THE EXTENT SUCH COVENANTS, CONDITIONS OR RESTRICTIONS VIOLATE TITLE 42, SECTION 3604(C), OF THE UNITED STATES CODES OR SECTION 12455 OF THE CALIFORNIA GOVERNMENT CODE. LAWFUL RESTRICTIONS UNDER STATE AND FEDERAL LAW ON THE AGE OF OCCUPANTS IN SENIOR HOUSING OR HOUSING FOR OLDER PERSONS SHALL NOT BE CONSTRUED AS RESTRICTIONS BASED ON FAMILIAL STATUS.
- 12. EASEMENTS, COVENANTS AND CONDITIONS CONTAINED IN THE DEED FROM ETHANAC RANCHO, LTD, AS GRANTOR, TO PHILA M. LAUDIG, AS GRANTEE, RECORDED APRIL 24, 1933 AS BOOK 123, PAGE 42 OF OFFICIAL RECORDS. REFERENCE BEING MADE TO THE DOCUMENT FOR FULL PARTICULARS. [BLANKET IN NATURE]
- 14. AN EASEMENT FOR TELEPHONE LINE, CONSISTING OF POLES, NECESSARY GUYS AND ANCHORS BRACES, CROSS-ARMS, WIRE, COUPLERS, CABLES, AND OTHER FIXTURES AND APPLIANCES FOR CONVEYING ELECTRIC ENERGY, TO BE USED FOR COMMUNICATION, TELEPHONE, TELEGRAPH AND/OR OTHER PURPOSES AND ACCESS FOR EXERCISING THE RIGHTS AND INCIDENTAL PURPOSES, RECORDED SEPTEMBER 18, 1961 AS INSTRUMENT NO. 194975, BOOK 2483, PAGE 446 OF OFFICIAL RECORDS, IN FAVOR OF CALIFORNIA WATER & TELEPHONE COMPANY, A CORPORATION, AND ITS SUCCESSORS.

LEGAL DESCRIPTION

APN: 329-240-016 (AFFECTS THE PORTION OF LOT 133), 329-240-017 (AFFECTS THE PORTION OF LOT 134), 329-240-018 (AFFECTS THE PORTION OF LOT 134), 329-240-019 (AFFECTS THE PORTION OF LOT 134), 329-240-020 (AFFECTS THE PORTION OF LOT 134), 329-240-026 (AFFECTS THE LOT 135), 329-240-021 (AFFECTS THE PORTION OF LOT 130), 329-240-025 (AFFECT LOT 136).

REAL PROPERTY IN THE CITY OF PERRIS, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, DESCRIBED AS FOLLOWS:

PARCEL A:
LOTS 134, 137 AND THE SOUTH 198 FEET OF LOTS 133 AND 138 OF ROMOLA FARMS NO. 6A, IN THE CITY OF PERRIS, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS SHOWN BY MAP ON FILE IN BOOK 14, PAGES 63, 64, AND 65 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

PARCEL B:
LOT 136 OF ROMOLA FARMS NO. 6A, IN THE CITY OF PERRIS, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS SHOWN BY MAP ON FILE IN BOOK 14, PAGES 63, 64, AND 65 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

APN: 329-240-023 (AFFECTS PARCEL A) AND 329-240-024 (AFFECTS PARCEL B)

PARCEL A:
THE WESTERLY 128.00 FEET OF THE EASTERLY 256.00 FEET OF LOT 135 OF ROMOLA FARMS NO. 6-A, IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 14, PAGES 63, 64 AND 65 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

PARCEL B:
THAT PORTION OF LOT 135 OF ROMOLA FARMS NO. 6A, AS SHOWN BY MAP OF SAID TRACT ON FILE IN BOOK 14, PAGES 63, 64, AND 65 OF MAPS, RIVERSIDE COUNTY RECORDS, DESCRIBED AS FOLLOWS:

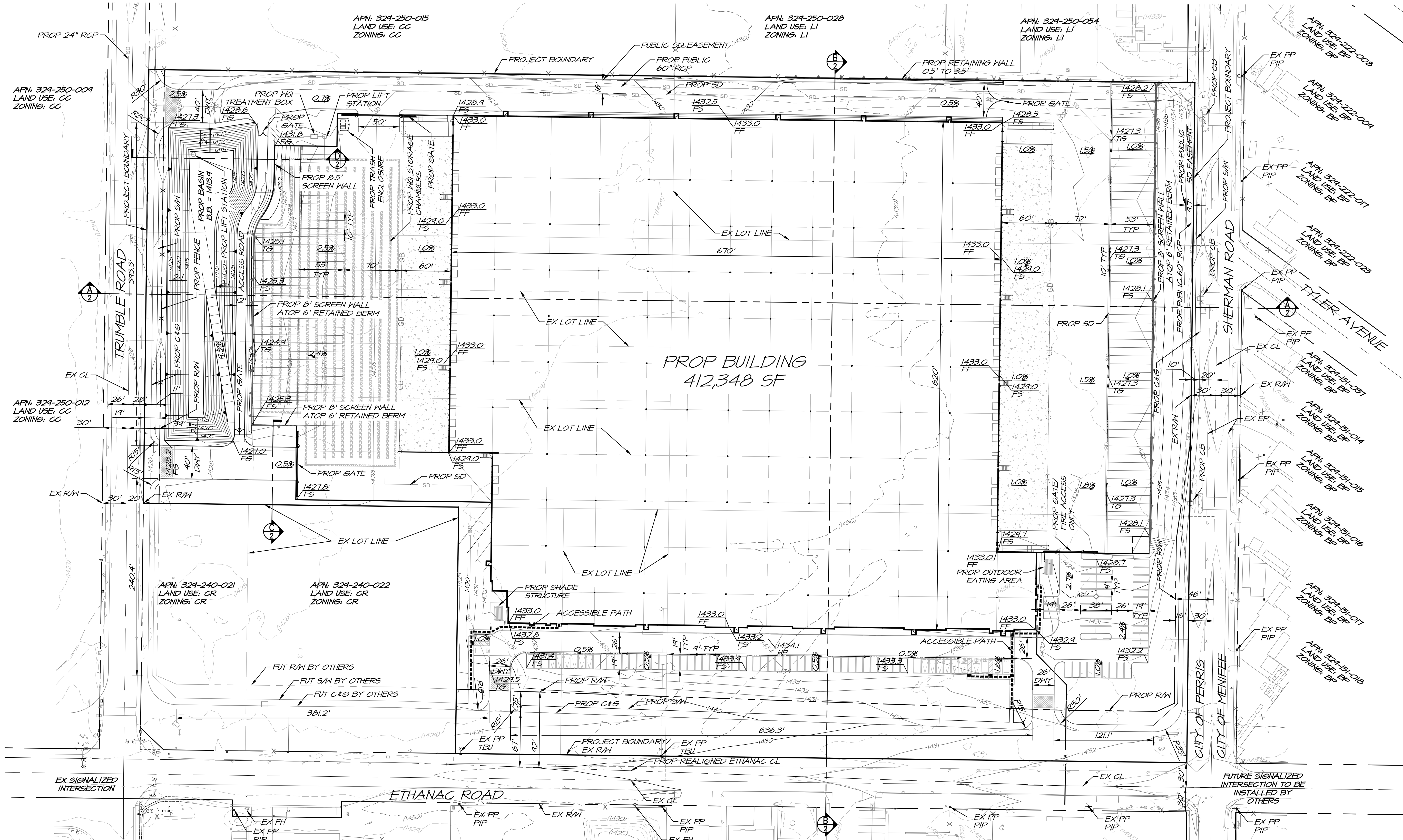
BEGINNING AT THE SOUTHEAST CORNER OF SAID LOT 135; THENCE WEST, ON THE SOUTH LINE OF SAID LOT, 128 FEET; THENCE NORTH, PARALLEL WITH THE EAST LINE OF SAID LOT, 300 FEET, TO A POINT ON THE NORTH LINE OF SAID LOT, 128 FEET WEST OF THE NORTHEAST CORNER THEREOF; THENCE EAST, ON THE NORTH LINE OF SAID LOT, 128 FEET; TO THE NORTHEAST CORNER THEREOF; THENCE SOUTH, ON THE EAST LINE OF SAID LOT, 300 FEET; TO THE POINT OF BEGINNING.

REVISED SECTIONS FOR PUBLIC DETENTION BASIN	4/23	AG
REVISIONS	DATE	BY

CITY OF PERRIS
ETHANAC COMMERCE CENTER
GRADING AND STREET SECTIONS
DEVELOPMENT PLAN REVIEW NO. 22-00030

SCALE: NTS	ALBERT A. WEBB ASSOCIATES	ENGINEERING CONSULTANTS 3785 MCGRAW STREET RIVERSIDE CA 92506 PH. (951) 686-1070 FAX (951) 788-1256	W.O. 22-0198
DATE: 4/17/2023	DESIGNED: AG	CHECKED: SK	SHEET 2
PLN CK REF: F.B.			OF 5 SHEETS
			DWG. NO.

PRELIMINARY
4/17/2023 2:16:15 PM Recce20



LEGEND

- (1430) --- EXISTING CONTOUR
- EXISTING CENTER LINE
- EXISTING EASEMENT
- EXISTING EDGE OF PAVEMENT
- X EXISTING FENCE
- EXISTING LOT LINE
- GB --- GRADE BREAK
- PROJECT BOUNDARY
- PROPOSED CENTER LINE
- (1430) --- PROPOSED CONTOUR
- PROPOSED CURB
- PROJ/EX RIGHT OF WAY

ABBREVIATIONS

- CB CATCH BASIN
- EP EDGE OF PAVEMENT
- EX EXISTING
- FF FINISHED FLOOR
- FH FIRE HYDRANT
- FS FINISHED SURFACE
- PIP PROTECT IN PLACE
- PROP PROPOSED
- R/W RIGHT-OF-WAY
- RCB REINFORCED CONCRETE BOX
- RCP REINFORCED CONCRETE PIPE
- S/W SIDEWALK
- TBU TO BE UNDERGROUNDED
- TG TOP OF GRATE
- TYP TYPICAL

APN: 331-110-004
LAND USE: CR
ZONING: CR

APN: 331-110-005
LAND USE: BP
ZONING: BP

APN: 331-110-006
LAND USE: BP
ZONING: BP

APN: 331-110-008
LAND USE: BP
ZONING: BP

APN: 331-110-009
LAND USE: BP
ZONING: BP

APN: 331-110-010
LAND USE: BP
ZONING: BP

APN: 331-110-011
LAND USE: BP
ZONING: BP

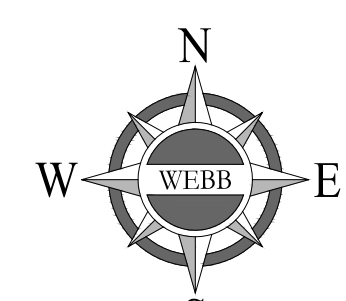
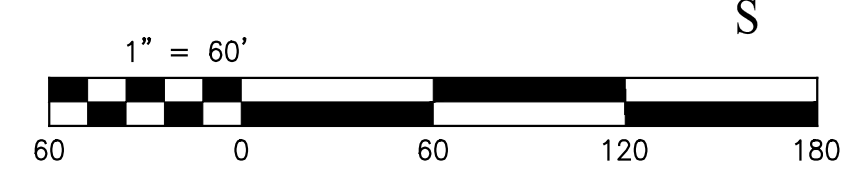
REVISIONS	DATE	BY
PUBLIC DETENTION BASIN ADDED	4/23	AG

CITY OF PERRIS
ETHANAC COMMERCE CENTER
 CONCEPTUAL GRADING
 DEVELOPMENT PLAN REVIEW NO. 22-00030

SCALE: 1" = 60'
 DATE: 4/17/2023
 DESIGNED: AG
 CHECKED: SK
 PLN CK REF: F.B.

ALBERT A. WEBB ASSOCIATES
 ENGINEERING CONSULTANTS
 3785 MCGRAY STREET
 RIVERSIDE, CA 92506
 PH. (951) 686-1070
 FAX (951) 788-1256

W.O. 22-0198
 SHEET 3
 OF 5 SHEETS
 DWG. NO.



PRELIMINARY

H:\2022\22-0198\DRAWINGS\ENTITLEMENT\22-0198-C-PP.DWG 4/17/2023 2:16:15 PM Rerecbb

SEE SHEET 5

APN: 324-250-004
LAND USE: CC
ZONING: CC

APN: 324-250-015
LAND USE: CC
ZONING: CC

APN: 324-250-028
LAND USE: LI
ZONING: LI

APN: 324-250-054
LAND USE: LI
ZONING: LI

APN: 324-222-008
LAND USE: BP
ZONING: BP

APN: 324-222-004
LAND USE: BP
ZONING: BP

APN: 324-222-017
LAND USE: BP
ZONING: BP

APN: 324-222-028
LAND USE: BP
ZONING: BP

APN: 324-151-097
LAND USE: BP
ZONING: BP

APN: 324-151-014
LAND USE: BP
ZONING: BP

APN: 324-151-015
LAND USE: BP
ZONING: BP

APN: 324-151-016
LAND USE: BP
ZONING: BP

APN: 324-151-017
LAND USE: BP
ZONING: BP

APN: 324-151-018
LAND USE: BP
ZONING: BP

APN: 324-151-019
LAND USE: BP
ZONING: BP

APN: 324-151-020
LAND USE: BP
ZONING: BP

APN: 324-250-012
LAND USE: CC
ZONING: CC

APN: 324-240-021
LAND USE: CR
ZONING: CR

APN: 324-240-022
LAND USE: CR
ZONING: CR

APN: 331-110-004
LAND USE: CR
ZONING: CR

APN: 331-110-005
LAND USE: BP
ZONING: BP

APN: 331-110-006
LAND USE: BP
ZONING: BP

APN: 331-110-008
LAND USE: BP
ZONING: BP

APN: 331-110-009
LAND USE: BP
ZONING: BP

APN: 331-110-010
LAND USE: BP
ZONING: BP

APN: 331-110-011
LAND USE: BP
ZONING: BP

APN: 331-114-001
LAND USE: BP
ZONING: BP

EX TELEPHONE
EX 2" GAS
EX ELECTRIC
EX 18" SEWER
EX 12" WATER

EX CL
EX R/W

EX SIGNALIZED INTERSECTION

LEGEND
18"RAW EXISTING 18" BRACKISH WATER
EXISTING BRACKISH WATER BLOW OFF VALVE
EXISTING ELECTRIC
EXISTING FIRE HYDRANT
EX 2"G EXISTING 2" GAS
EX 3"G EXISTING 3" GAS
EX 6"G EXISTING 6" GAS
EXISTING OVERHEAD UTILITY
6"SS EXISTING 6" SEWER
8"SS EXISTING 8" SEWER
15"SS EXISTING 15" SEWER
18"SS EXISTING 18" SEWER
EXISTING SEWER MANHOLE
36"SD EXISTING 36" STORM DRAIN
48"SD EXISTING 48" STORM DRAIN
EXISTING TELEPHONE
8"W EXISTING 8" WATER
12"W EXISTING 12" WATER
20"W EXISTING 20" WATER
PROJECT BOUNDARY

RIGHT-OF-WAY (EXISTING/PROPOSED)
W PROPOSED WATER
FIRE PROPOSED FIRE LINE
SS PROPOSED SEWER
SD PROPOSED STORM DRAIN

ABBREVIATIONS
CL CENTER LINE
C&G CURB AND GUTTER
EX EXISTING
PP POWER POLE
PROP PROPOSED
PIP PROTECT IN PLACE
RCB REINFORCED CONCRETE BOX
RCP REINFORCED CONCRETE PIPE
R/W RIGHT-OF-WAY
SD STORM DRAIN
TBU TO BE UNDERGROUNDED
TYP TYPICAL

PROP BUILDING
412,348 SF
1433.0 F.F.

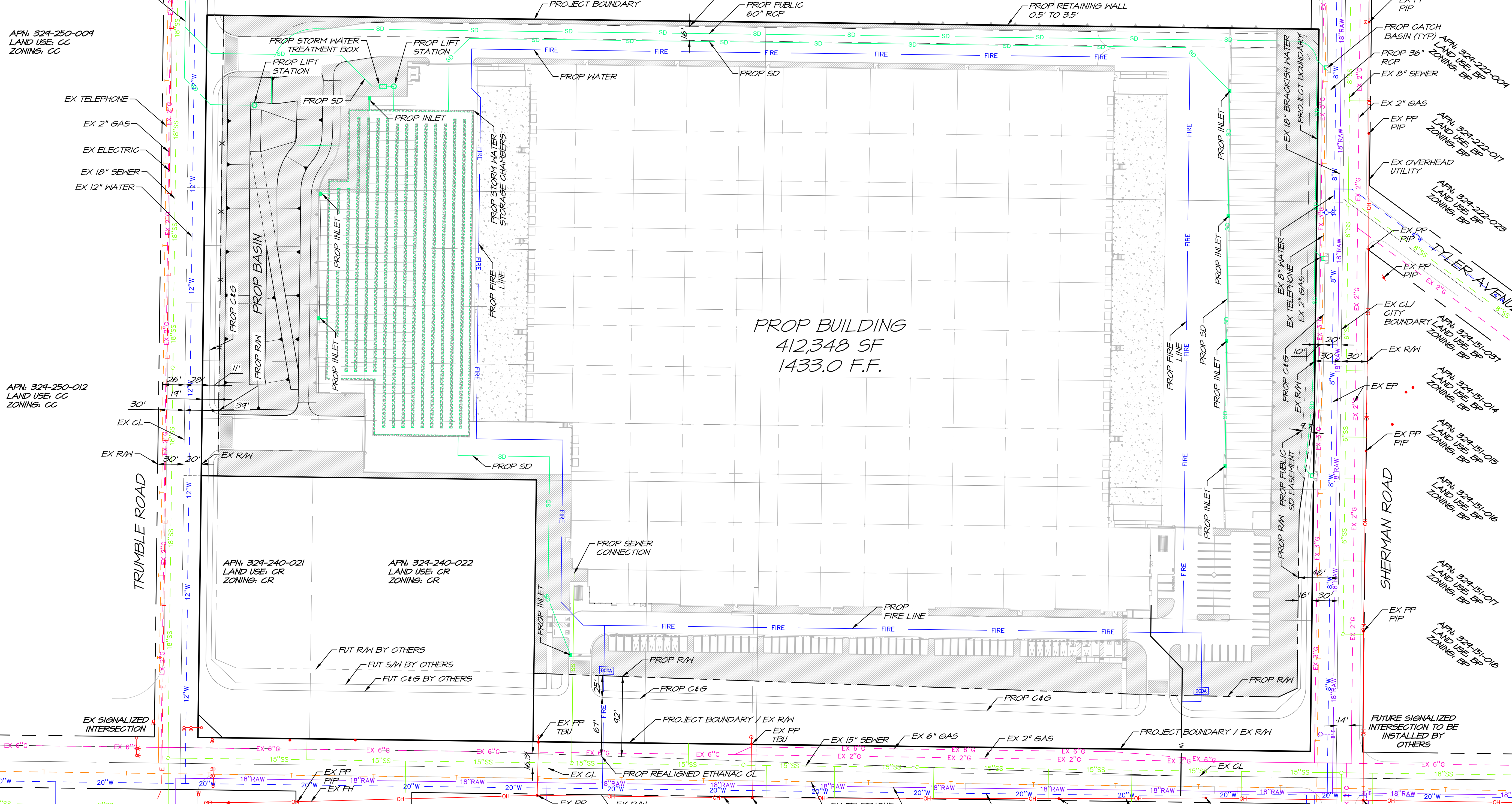


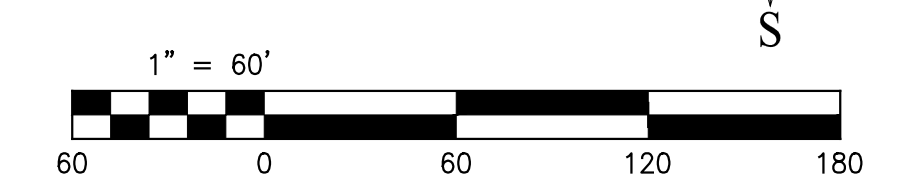
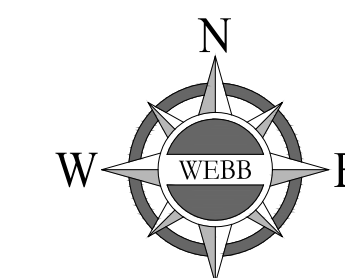
Table with 3 columns: REVISIONS, DATE, BY. Row 1: PUBLIC DETENTION BASIN ADDED, 4/23, AG.

CITY OF PERRIS
ETHANAC COMMERCE CENTER
CONCEPTUAL UTILITY
DEVELOPMENT PLAN REVIEW NO. 22-00030

SCALE: 1" = 60'
DATE: 4/17/2023
DESIGNED: AG
CHECKED: SK
PLN CK REF:
F.B.

ALBERT A. WEBB ASSOCIATES
ENGINEERING CONSULTANTS
3785 MCGRAY STREET
RIVERSIDE CA 92506
PH. (951) 686-1070
FAX (951) 788-1256

W.O. 22-0192
SHEET 4
OF 5 SHEETS
DWG. NO.



PRELIMINARY

4/14/2023 2:56:04 PM Admin



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SEE SHEET 4

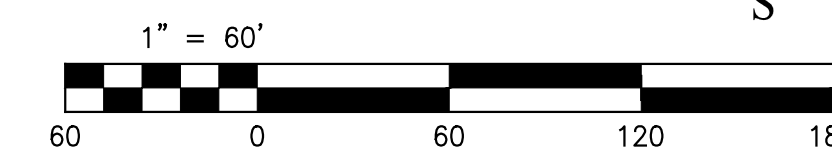
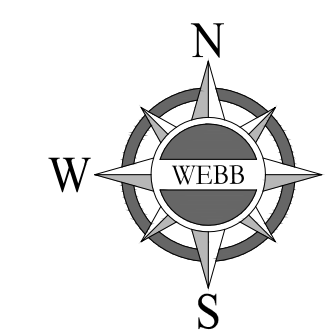
LEGEND

- 18"RAW — EXISTING 18" BRACKISH WATER
- ⊕ — EXISTING BRACKISH WATER BLOW OFF VALVE
- E — EXISTING ELECTRIC
- ⊕ — EXISTING FIRE HYDRANT
- EX 2"G — EXISTING 2" GAS
- EX 3"G — EXISTING 3" GAS
- EX 6"G — EXISTING 6" GAS
- OH — EXISTING OVERHEAD UTILITY
- 6"SS — EXISTING 6" SEWER
- 8"SS — EXISTING 8" SEWER
- 15"SS — EXISTING 15" SEWER
- 18"SS — EXISTING 18" SEWER
- ⊙ — EXISTING SEWER MANHOLE
- 36"SD — EXISTING 36" STORM DRAIN
- 48"SD — EXISTING 48" STORM DRAIN
- T — EXISTING TELEPHONE
- 8"W — EXISTING 8" WATER
- 12"W — EXISTING 12" WATER
- 20"W — EXISTING 20" WATER
- — PROJECT BOUNDARY

- — — — — RIGHT-OF-WAY (EXISTING/PROPOSED)
- W — PROPOSED WATER
- FIRE — PROPOSED FIRE LINE
- SS — PROPOSED SEWER
- SD — PROPOSED STORM DRAIN

ABBREVIATIONS

- CL — CENTER LINE
- C&G — CURB AND GUTTER
- EX — EXISTING
- PP — POWER POLE
- PROP — PROPOSED
- PIP — PROTECT IN PLACE
- RCB — REINFORCED CONCRETE BOX
- RCP — REINFORCED CONCRETE PIPE
- R/W — RIGHT-OF-WAY
- SD — STORM DRAIN
- TBU — TO BE UNDERGROUNDED
- TYP — TYPICAL

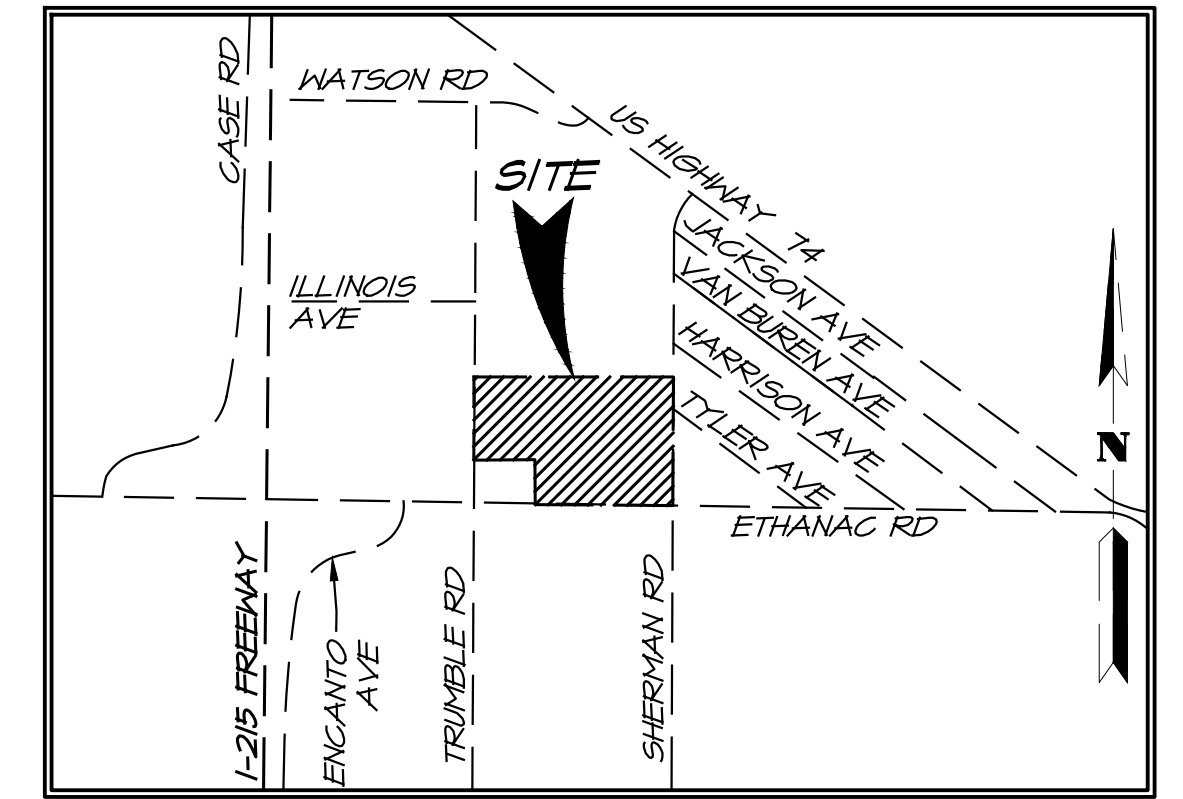


REFERENCE TO MDP LINE A-II ADDED	DATE	BY
	4/23	AG
REVISIONS		
	DATE	BY

CITY OF PERRIS		
ETHANAC COMMERCE CENTER CONCEPTUAL UTILITY DEVELOPMENT PLAN REVIEW NO. 22-00030		
SCALE: 1" = 60'	DATE: 4/17/2023	W.O. 22-0192
DESIGNED: AG	CHECKED: SK	SHEET 5
PLN CK REF:	F.B.	OF 5 SHEETS
		ENGINEERING CONSULTANTS 3785 MCGRAY STREET RIVERSIDE CA 92506 PH. (951) 686-1070 FAX (951) 788-1256
		DWG. NO.

PRELIMINARY
H:\2022\22-0192\DRAWINGS\ENTITLEMENT\22-0192-C-PP-CONCEPTUAL UTILITIES.DWG 4/14/2023 2:56:04 PM Adamg

IN THE CITY OF PERRIS, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA
TENTATIVE PARCEL MAP NO. 38600
 LOCATED IN SECTION 11, T. 4S., R. 4W., S.B.M.



VICINITY MAP
 NTS

OWNER/APPLICANT
 HILLWOOD
 901 VIA PIEMONTE STE. 175
 ONTARIO CA, 91764
 CONTACT: JOHN GRACE
 EMAIL: JOHN.GRACE@HILLWOOD.COM
 PHONE: (909) 256-5424

PROJECT REPRESENTATIVE
 ALBERT A. WEBB ASSOCIATES
 3788 MCCRAY STREET
 RIVERSIDE, CA 92506
 CONTACT: RICHARD BELMUEZ
 PHONE: (951) 686-1070
 FAX: (951) 788-1256

SOILS ENGINEER
 SOUTHERN CALIFORNIA GEOTECHNICAL
 22885 E. SAVI RANCH PARKWAY STE. E
 RIVERSIDE, CA 92506
 CONTACT: ROBERT TRAZO
 PHONE: (714) 685-1115
 FAX: (714) 685-1118

ENGINEER
 ALBERT A. WEBB ASSOCIATES
 3788 MCCRAY STREET
 RIVERSIDE, CA 92506
 CONTACT: SARAH KOWALSKI
 PHONE: (951) 686-1070
 FAX: (951) 788-1256

TOPOGRAPHY SOURCE
 TOPOGRAPHY FLOWN BY INLAND
 AERIAL SURVEYS, INC. ON 02/15/2022

ARCHITECT
 HPA ARCHITECTURE
 18831 BARDEEN AVE., STE 100
 IRVINE, CA 92612
 CONTACT: INKON KIM
 PHONE: (949) 863-1770
 EMAIL: JAIME@HPARCHS.COM

A.P.N.
 324-240-016 THRU -020, -023,
 THRU -027

ACREAGE

GROSS	21.6 AC
NET	19.9 AC
R/W	1.7 AC

LAND USE
 EXISTING LAND USE: VACANT
 EXISTING GENERAL PLAN LAND USE:
 COMMERCIAL COMMUNITY (CC)
 EXISTING ZONING: LIGHT INDUSTRIAL (LI)
 PROPOSED GENERAL PLAN LAND USE:
 COMMERCIAL COMMUNITY (CC)
 PROPOSED ZONING: LIGHT INDUSTRIAL (LI)

PROJECT DATA

BUILDING AREA	10,000 SF
OFFICE - FIRST FLOOR	5,000 SF
OFFICE - SECOND FLOOR	3,134.8 SF
WAREHOUSE	407,348 SF
TOTAL BUILDING FOOTPRINT	412,348 SF
TOTAL FLOOR AREA	412,348 SF

AUTO PARKING REQUIRED

OFFICE:	(1 STALL/300 SF IF LESS THAN 10% GFA)	N/A
WAREHOUSE:	105 STALLS @ (30 STALLS + 1 STALL/5,000 SF)	105 STALLS
TOTAL		105 STALLS

AUTO PARKING PROVIDED

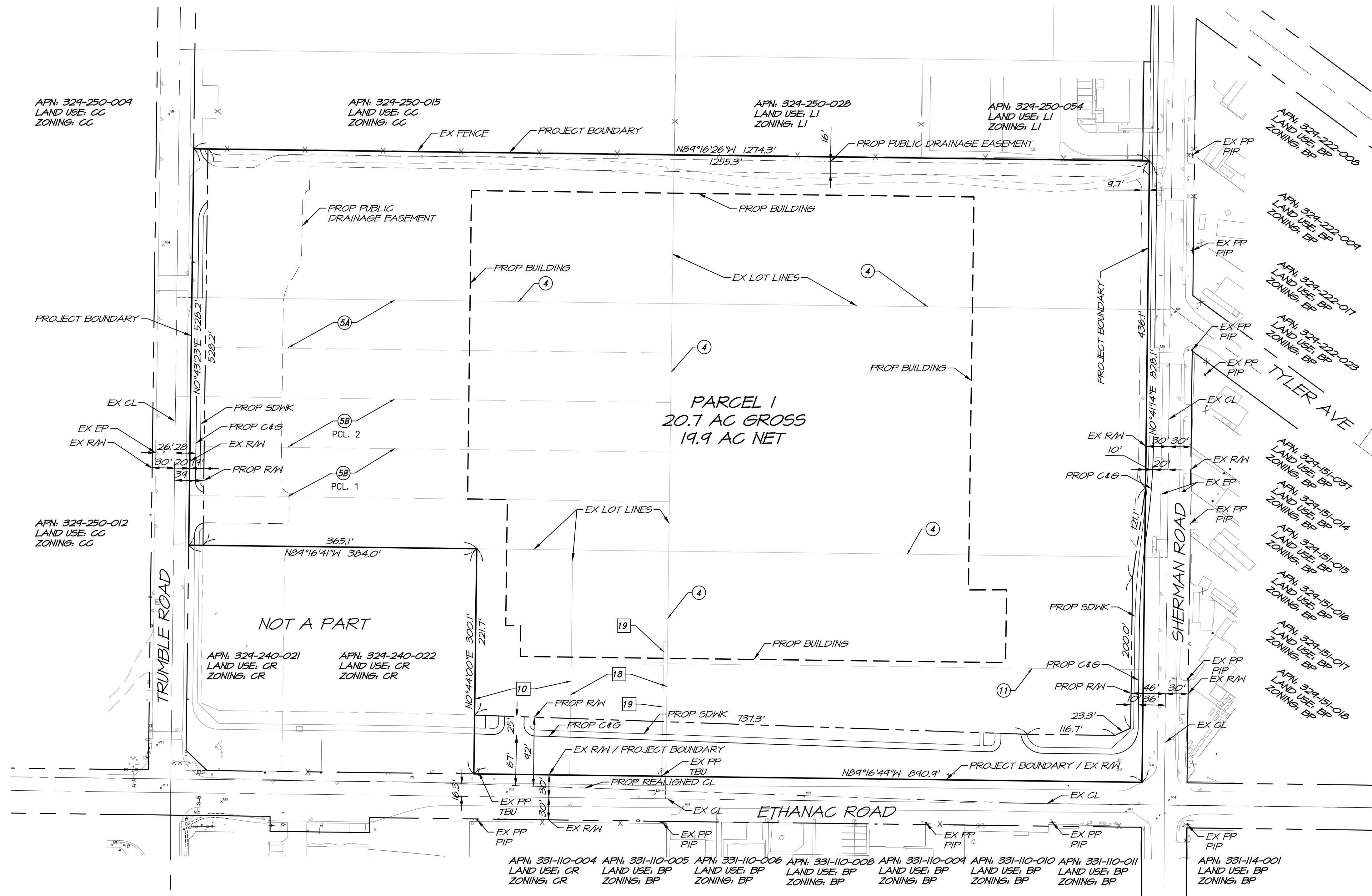
STANDARD (9' x 19')	83 STALLS
ADA STANDARD (9' x 19')	3 STALLS
ADA VAN ACCESSIBLE (12' x 19')	2 STALLS
EV STANDARD (9' x 19')	11 STALLS
EV ADA STANDARD (9' x 19')	1 STALL
EV ADA VAN ACCESSIBLE (12' x 19')	5 STALLS
CLEAN AIR/VAN/POOL/EV (9' x 19')	106 STALLS
TOTAL	106 STALLS

TRAILER PARKING REQUIRED

17,500 SF	82 STALLS
-----------	-----------

TRAILER PARKING PROVIDED

TYPICAL TRAILER (10' x 35')	112 STALLS
-----------------------------	------------



LEGEND

---	EXISTING CENTER LINE
- - -	EXISTING EASEMENT
- - - -	EXISTING EDGE OF PAVEMENT
- x -	EXISTING FENCE
- - - -	EXISTING LOT LINE
- - - -	PROJECT BOUNDARY
- - - -	PROPOSED RIGHT OF WAY
- - - -	PROPOSED BUILDING OUTLINE
- - - -	PROPOSED CENTER LINE
- - - -	PROPOSED CURB

ABBREVIATIONS

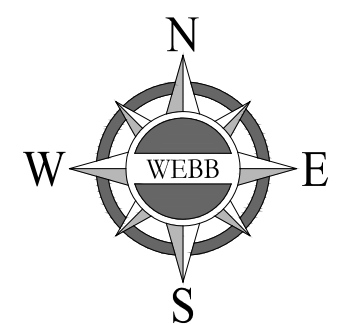
CL	CENTER LINE
C&G	CURB AND GUTTER
EP	EDGE OF PAVEMENT
EX	EXISTING
PP	POWER POLE
PROP	PROPOSED
PIP	PROTECT IN PLACE
R/W	RIGHT-OF-WAY
S/W	SIDEWALK
TBR	TO BE REMOVED
TBU	TO BE UNDERGROUND

- GENERAL INFORMATION**
- ALL PARCELS WITHIN PROJECT BOUNDARY TO BE COMBINED VIA PARCEL MAP
 - THOMAS BROS. MAP BOOK PAGE: B38, GRID: C1 AND D1
 - PROJECT IS NOT WITHIN A SPECIFIC PLAN.
 - PROJECT LIES WITHIN THE CITY OF PERRIS REDEVELOPMENT PROJECT AREA.
 - PROJECT LIES WITHIN CFD NO. 1.
 - EASEMENTS OF RECORD ARE PLOTTED HEREIN.
 - PROJECT IS WITHIN EASTERN MUNICIPAL WATER DISTRICT
 - THERE ARE NO EXISTING WELLS ON THE PROPERTY.
 - ALL SLOPES ARE 2:1 RATIO, UNLESS OTHERWISE NOTED.
 - LAND IS NOT WITHIN A SPECIAL STUDIES ZONE.
 - LAND HAS LOW POTENTIAL FOR LIQUEFACTION PER SOCAL GEO REPORT DATED 02/23/2022.
 - STRUCTURES AND/OR DWELLINGS DO NOT EXIST ON SITE.
 - THE PROJECT WILL COMPLY WITH NPDES REQUIREMENTS AS REQUIRED BY NPDES SUPPLEMENT "A".
 - FLOOD ZONE X, AREA OF LOW FLOODING PER FEMA PANEL 06065C2060H.
 - PROJECT LIES WITHIN AIRPORT LAND USE COMPATIBILITY ZONE D
 - ARCHITECTURAL SITE PLAN PROVIDED BY HPA ARCHITECTURE ON 04/12/23

EASEMENT NOTES
 SEE SHEET 2

LEGAL DESCRIPTION
 SEE SHEET 2

SHEET INDEX
 SHEET 1: TENTATIVE PARCEL MAP
 SHEET 2: SECTIONS AND LEGAL



PROPOSED DRAINAGE EASEMENT ADDED	4/23	AG

CITY OF PERRIS

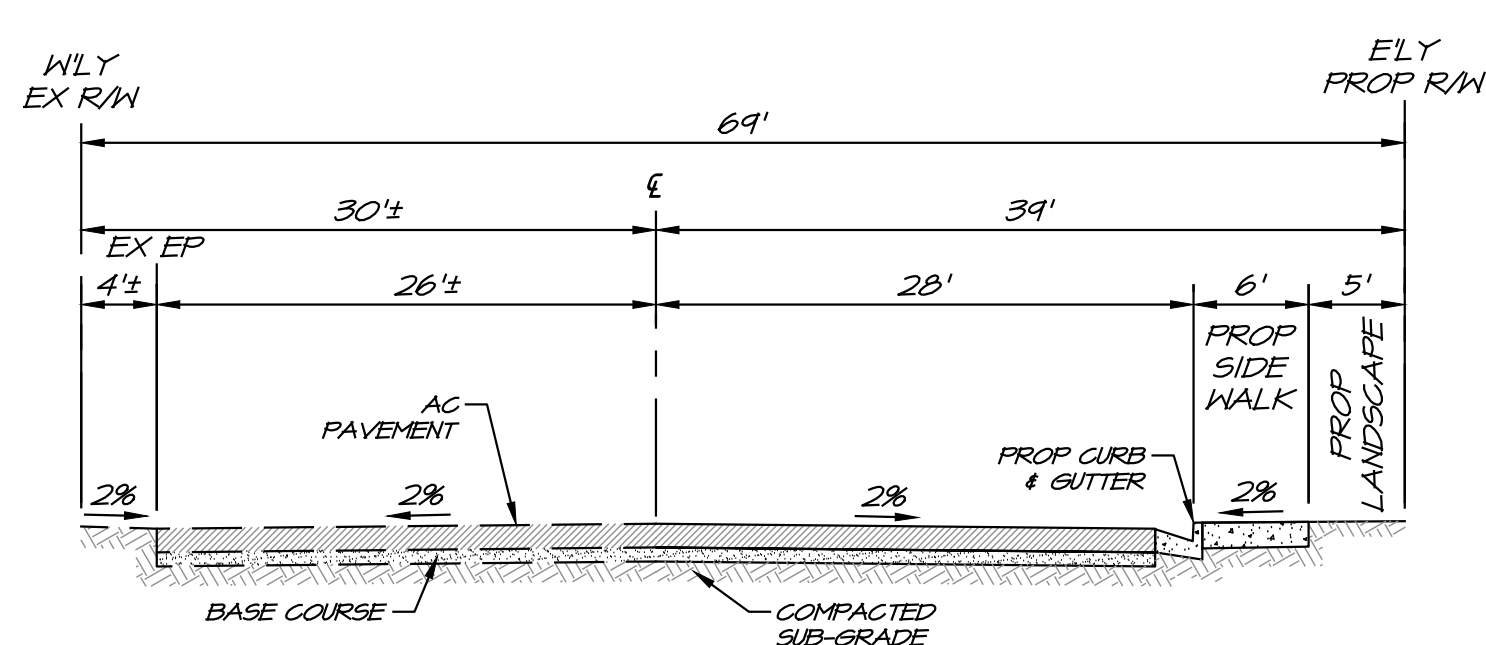
ETHANAC COMMERCE CENTER

TITLE SHEET

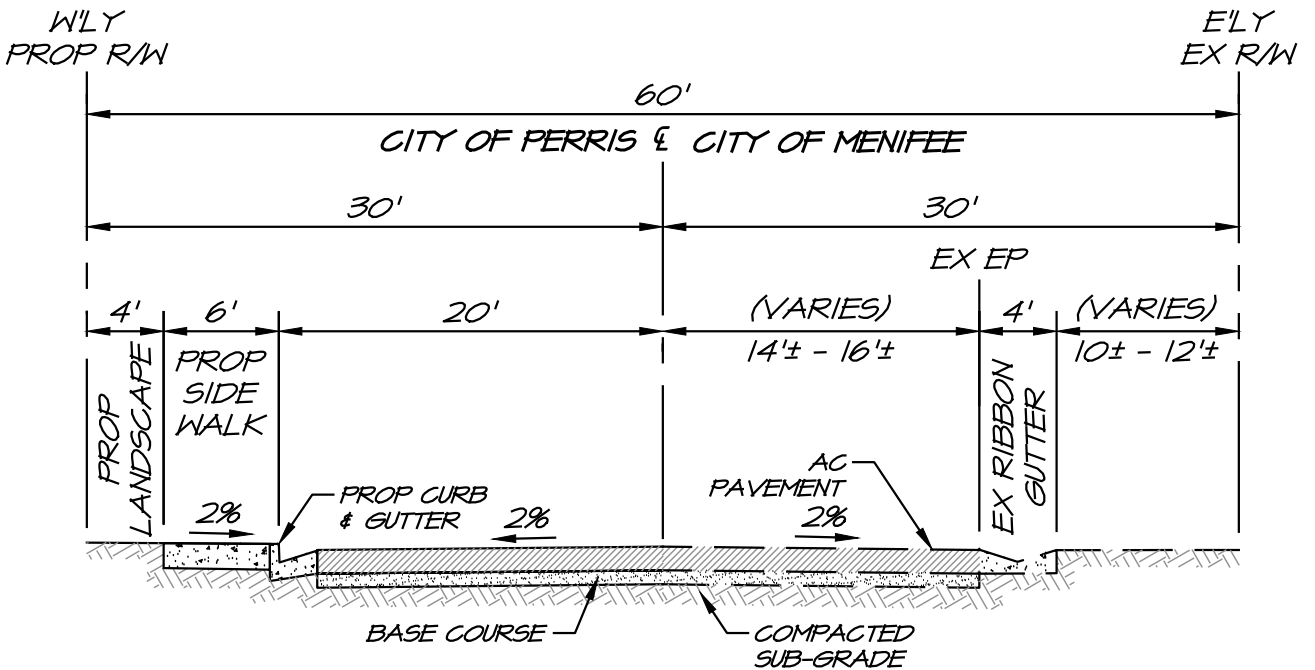
TENTATIVE PARCEL MAP NO. 38600

SCALE: 1" = 80'	 ENGINEERING CONSULTANTS 3788 MCCRAY STREET RIVERSIDE, CA 92506 PH. (951) 686-1070 FAX (951) 788-1256	W.O. 22-0192
DATE: 4/14/2023		SHEET 1
DESIGNED: AG		OF 2 SHEETS
CHECKED: SK		DWG. NO.

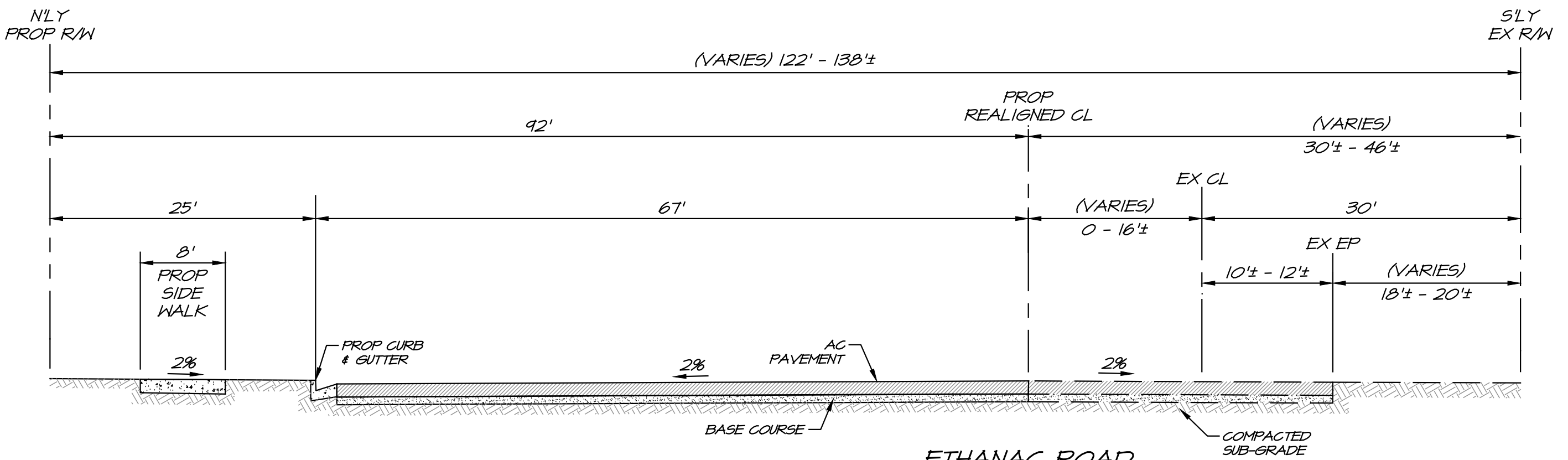
PRELIMINARY



TRUMBLE ROAD
MAJOR COLLECTOR
18' R/W
NTS



SHERMAN ROAD
LOCAL
60' R/W
NTS



ETHANAC ROAD
8-LANE EXPRESSWAY
184' R/W
NTS

EASEMENT NOTES

- APN: 329-240-016 THRU -020 AND 329-240-025 THRU -027.
- (4) AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED JUNE 25, 1927 IN BOOK 120 OF DEEDS, PAGE 209, IN FAVOR OF SOUTHERN SIERRAS POWER COMPANY. THE LOCATION OF THE EASEMENT CANNOT BE DETERMINED FROM RECORD INFORMATION.
 5. COVENANTS, CONDITIONS, RESTRICTIONS AND EASEMENTS IN THE DOCUMENT RECORDED APRIL 17, 1934 AS BOOK 170, PAGE 305 AND JANUARY 13, 1936 IN BOOK 259, PAGE 560 (BOTH) OF OFFICIAL RECORDS, BUT DELETING ANY COVENANT, CONDITION OR RESTRICTION INDICATING A PREFERENCE, LIMITATION OR DISCRIMINATION BASED ON RACE, COLOR, RELIGION, SEX, HANDICAP, FAMILIAL STATUS, NATIONAL ORIGIN, SEXUAL ORIENTATION, MARITAL STATUS, ANCESTRY, SOURCE OF INCOME OR DISABILITY, TO THE EXTENT SUCH COVENANTS, CONDITIONS OR RESTRICTIONS VIOLATE TITLE 42, SECTION 3604(C), OF THE UNITED STATES CODES. LAWFUL RESTRICTIONS UNDER STATE AND FEDERAL LAW ON THE AGE OF OCCUPANTS IN SENIOR HOUSING OR HOUSING FOR OLDER PERSONS SHALL NOT BE CONSTRUED AS RESTRICTIONS BASED ON FAMILIAL STATUS.
[(5A) BOOK 170, PAGE 305 & (5B) BOOK 259, PAGE 560]
 - (11) AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED AUGUST 8, 1947 AS BOOK 846, PAGE 355 OF OFFICIAL RECORDS, IN FAVOR OF CALIFORNIA ELECTRIC POWER COMPANY.
[& OF UNDISCLOSED WIDTH]
- APN: 329-240-023 AND -024
- (10) COVENANTS, CONDITIONS, RESTRICTIONS AND EASEMENTS IN THE DOCUMENT RECORDED APRIL 24, 1933 AS BOOK 120, PAGE 397 OF OFFICIAL RECORDS, WHICH PROVIDE THAT A VIOLATION THEREOF SHALL NOT DEFEAT OR RENDER INVALID THE LIEN OF ANY FIRST MORTGAGE OR DEED OF TRUST MADE IN GOOD FAITH AND FOR VALUE, BUT DELETING ANY COVENANT, CONDITION OR RESTRICTION INDICATING A PREFERENCE, LIMITATION OR DISCRIMINATION BASED ON RACE, COLOR, RELIGION, SEX, HANDICAP, FAMILIAL STATUS, NATIONAL ORIGIN, SEXUAL ORIENTATION, MARITAL STATUS, ANCESTRY, SOURCE OF INCOME OR DISABILITY, TO THE EXTENT SUCH COVENANTS, CONDITIONS OR RESTRICTIONS VIOLATE TITLE 42, SECTION 3604(C), OF THE UNITED STATES CODES OR SECTION 12955 OF THE CALIFORNIA GOVERNMENT CODE. LAWFUL RESTRICTIONS UNDER STATE AND FEDERAL LAW ON THE AGE OF OCCUPANTS IN SENIOR HOUSING OR HOUSING FOR OLDER PERSONS SHALL NOT BE CONSTRUED AS RESTRICTIONS BASED ON FAMILIAL STATUS.
 - (12) EASEMENTS, COVENANTS AND CONDITIONS CONTAINED IN THE DEED FROM ETHANAC RANCHO, LTD, AS GRANTOR, TO PHILA M. LAUDIG, AS GRANTEE, RECORDED APRIL 24, 1933 AS BOOK 123, PAGE 42 OF OFFICIAL RECORDS. REFERENCE BEING MADE TO THE DOCUMENT FOR FULL PARTICULARS. [BLANKET IN NATURE]
 - (19) AN EASEMENT FOR TELEPHONE LINE, CONSISTING OF POLES, NECESSARY GUYS AND ANCHORS, BRACES, CROSS-ARMS, WIRE, CONDUITS, CABLES, AND OTHER FIXTURES AND APPLIANCES FOR CONVEYING ELECTRIC ENERGY TO BE USED FOR COMMUNICATION, TELEPHONE, TELEGRAPH AND/OR OTHER PURPOSES AND ACCESS FOR EXERCISING THE RIGHTS AND INCIDENTAL PURPOSES, RECORDED SEPTEMBER 18, 1961 AS INSTRUMENT NO. 19475, BOOK 2183, PAGE 446 OF OFFICIAL RECORDS, IN FAVOR OF CALIFORNIA WATER & TELEPHONE COMPANY, A CORPORATION, AND ITS SUCCESSORS.

LEGAL DESCRIPTION

APN: 329-240-016 (AFFECTS THE PORTION OF LOT 133), 329-240-017 (AFFECTS THE PORTION OF LOT 134), 329-240-018 (AFFECTS THE PORTION OF LOT 134), 329-240-019 (AFFECTS THE PORTION OF LOT 134), 329-240-020 (AFFECTS THE PORTION OF LOT 134), 329-240-026 (AFFECTS THE LOT 135), 329-240-021 (AFFECTS THE PORTION OF LOT 135), 329-240-025 (AFFECT LOT 136).

REAL PROPERTY IN THE CITY OF PERRIS, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, DESCRIBED AS FOLLOWS:

PARCEL A:
LOTS 134, 137 AND THE SOUTH 198 FEET OF LOTS 133 AND 138 OF RAMOLA FARMS NO. 6A, IN THE CITY OF PERRIS, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS SHOWN BY MAP ON FILE IN BOOK 14, PAGES 63, 64, AND 65 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

PARCEL B:
LOT 136 OF RAMOLA FARMS NO. 6A, IN THE CITY OF PERRIS, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS SHOWN BY MAP ON FILE IN BOOK 14, PAGES 63, 64, AND 65 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

APN: 329-240-023 (AFFECTS PARCEL A) AND 329-240-024 (AFFECTS PARCEL B)

PARCEL A:

THE WESTERLY 128.00 FEET OF THE EASTERLY 256.00 FEET OF LOT 135 OF ROMOLA FARMS NO. 6-A, IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 14, PAGES 63, 64 AND 65 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

PARCEL B:

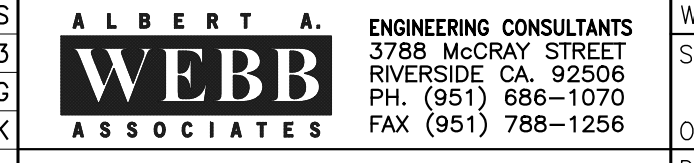
THAT PORTION OF LOT 135 OF ROMOLA FARMS NO. 6A, AS SHOWN BY MAP OF SAID TRACT ON FILE IN BOOK 14, PAGES 63, 64, AND 65 OF MAPS, RIVERSIDE COUNTY RECORDS, DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHEAST CORNER OF SAID LOT 135; THENCE WEST, ON THE SOUTH LINE OF SAID LOT, 128 FEET; THENCE NORTH, PARALLEL WITH THE EAST LINE OF SAID LOT, 300 FEET, TO A POINT ON THE NORTH LINE OF SAID LOT, 128 FEET WEST OF THE NORTHEAST CORNER THEREOF; THENCE EAST, ON THE NORTH LINE OF SAID LOT, 128 FEET, TO THE NORTHEAST CORNER THEREOF; THENCE SOUTH, ON THE EAST LINE OF SAID LOT, 300 FEET, TO THE POINT OF BEGINNING.

REVISIONS	DATE	BY

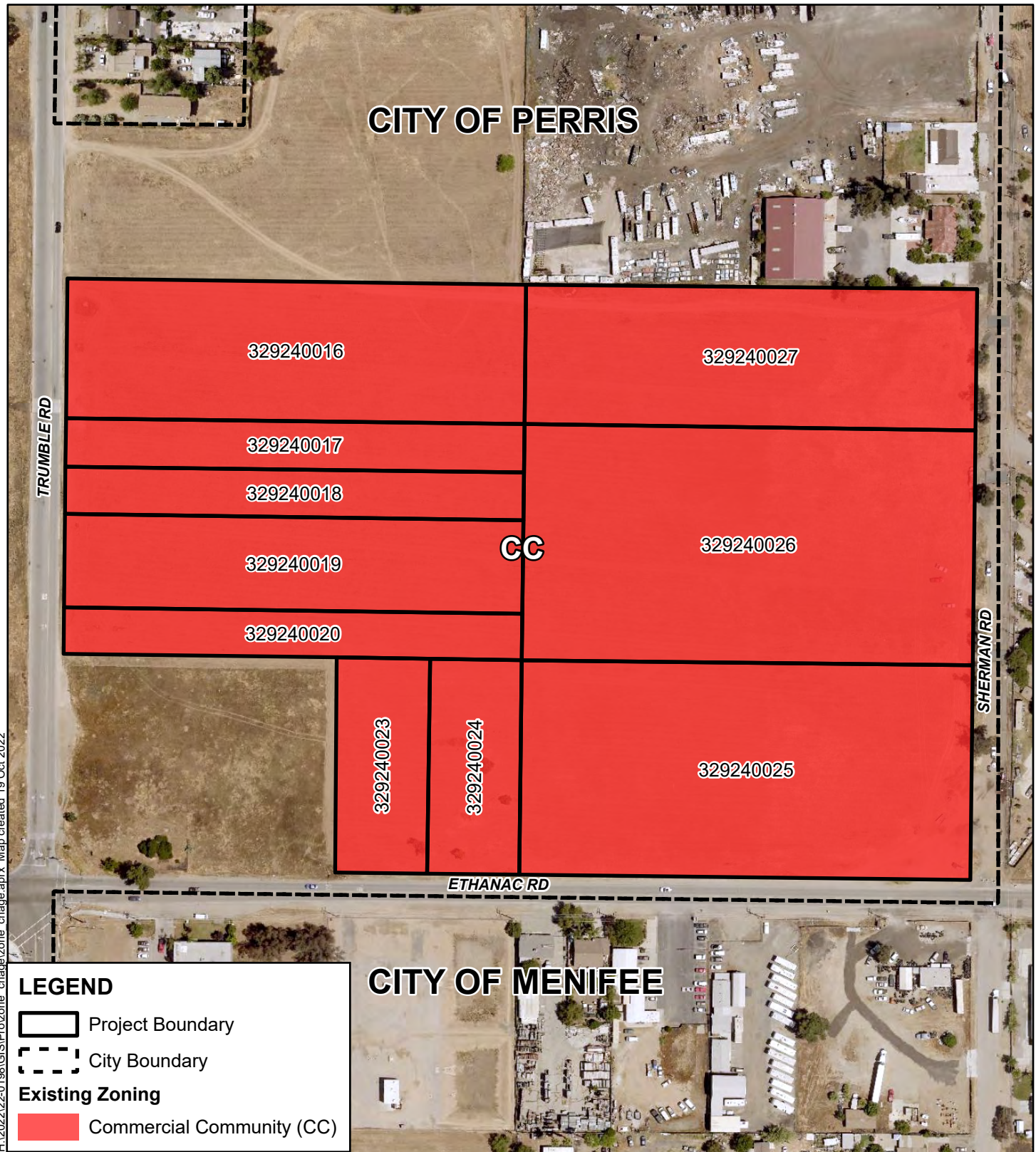
CITY OF PERRIS
ETHANAC COMMERCE CENTER
STREET SECTIONS AND LEGAL
TENTATIVE PARCEL MAP NO. 38600

SCALE: NTS	ENGINEERING CONSULTANTS	W.O. 22-0192
DATE: 4/14/2023	3785 MCGRAW STREET	SHEET 2
DESIGNED: AG	RIVERSIDE, CA 92506	OF 2 SHEETS
CHECKED: SK	PH. (951) 686-1070	DWG. NO.
PLN CK REF:	FAX (951) 788-1256	
F.B.		



PRELIMINARY
4/14/2023 7:45:11 PM Adams

H:\2022\22-0198\GIS\Pro\zone_change.aprx Map created 19 Oct 2022



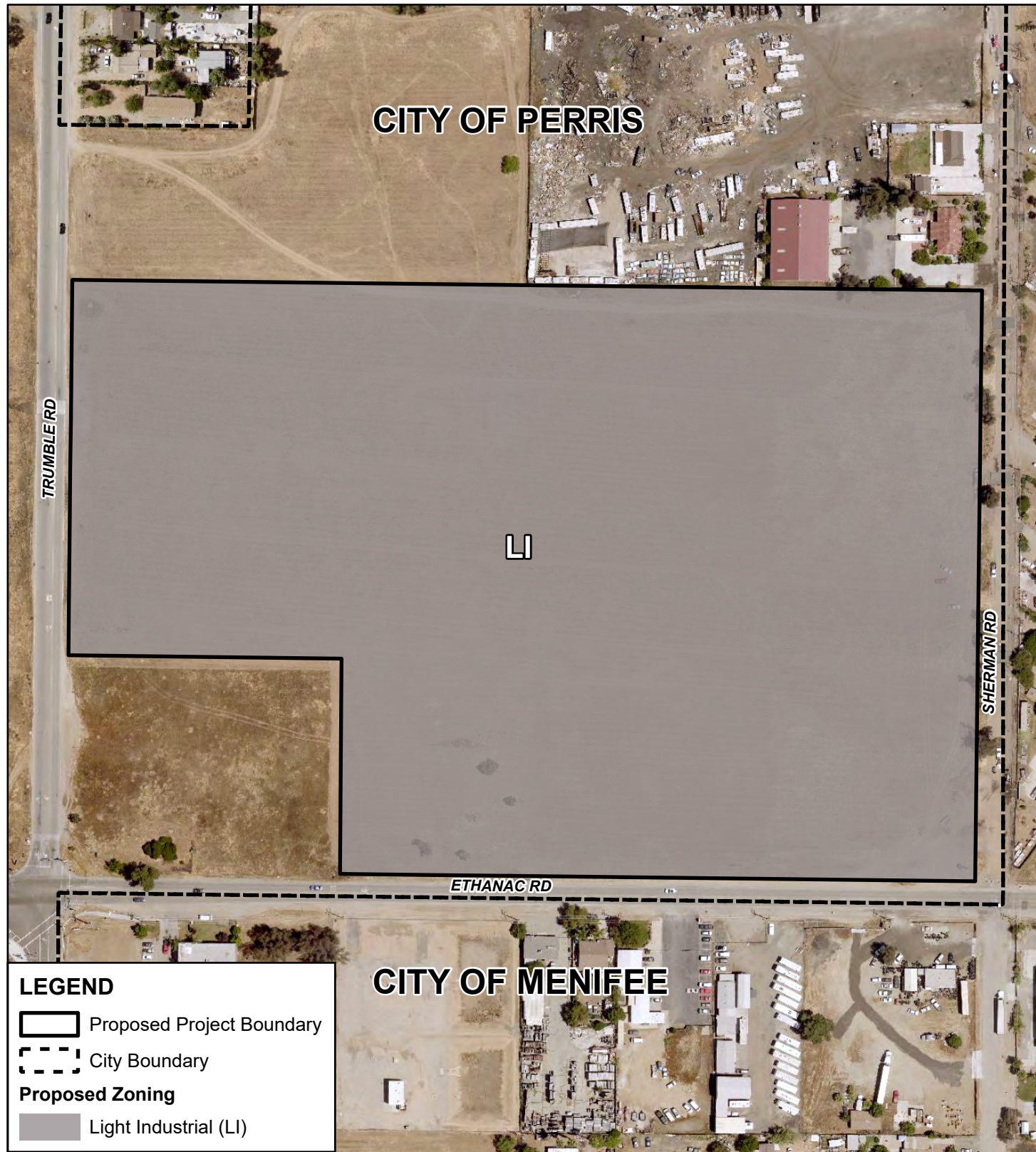
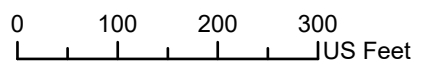
LEGEND

- Project Boundary
- City Boundary

Existing Zoning

- Commercial Community (CC)

Source: Riverside County 2020; City of Perris, Zoning, 2017.



LEGEND

- Proposed Project Boundary
- City Boundary

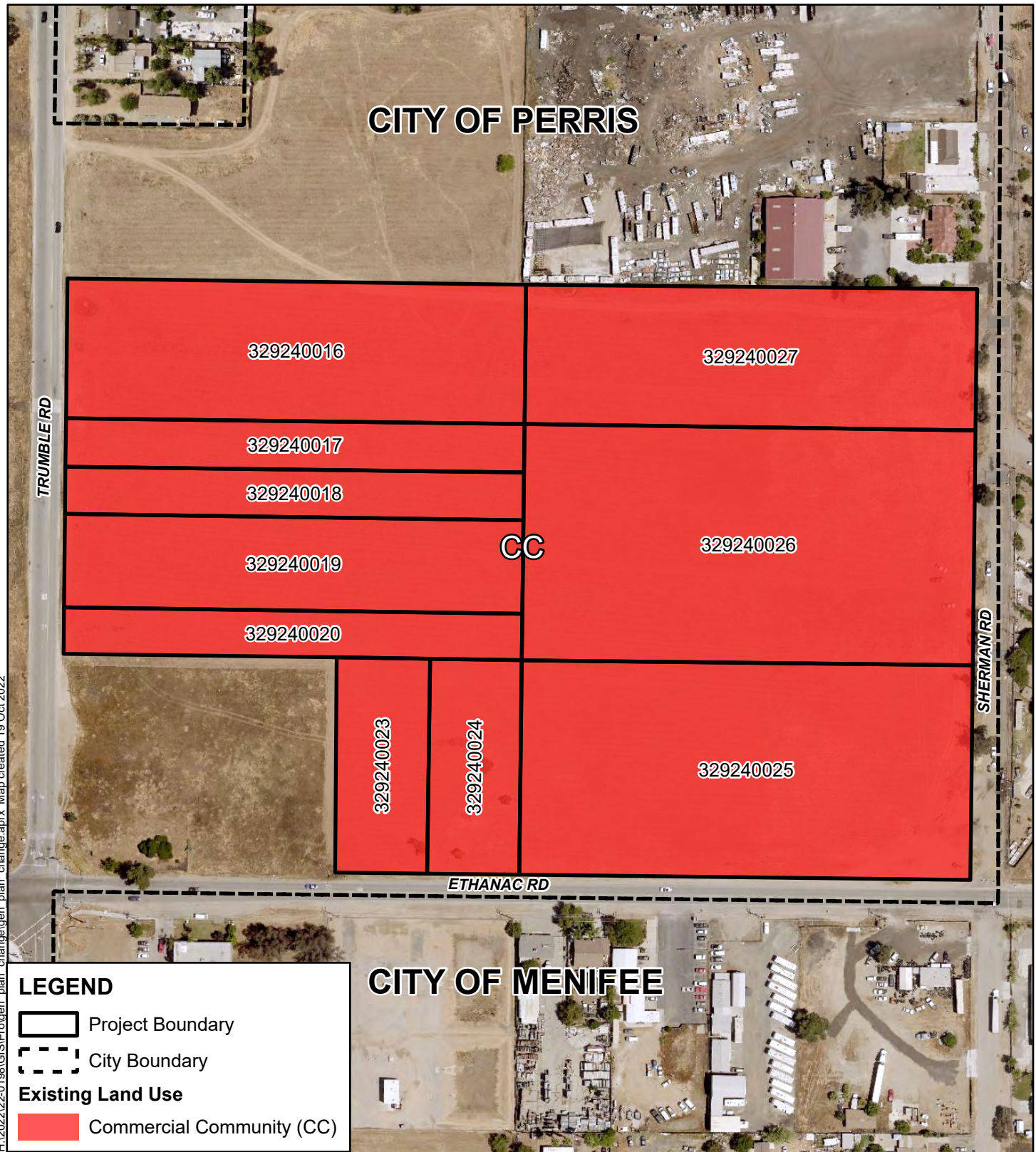
Proposed Zoning

- Light Industrial (LI)

Proposed Change of Zone
Hillwood Ethanac



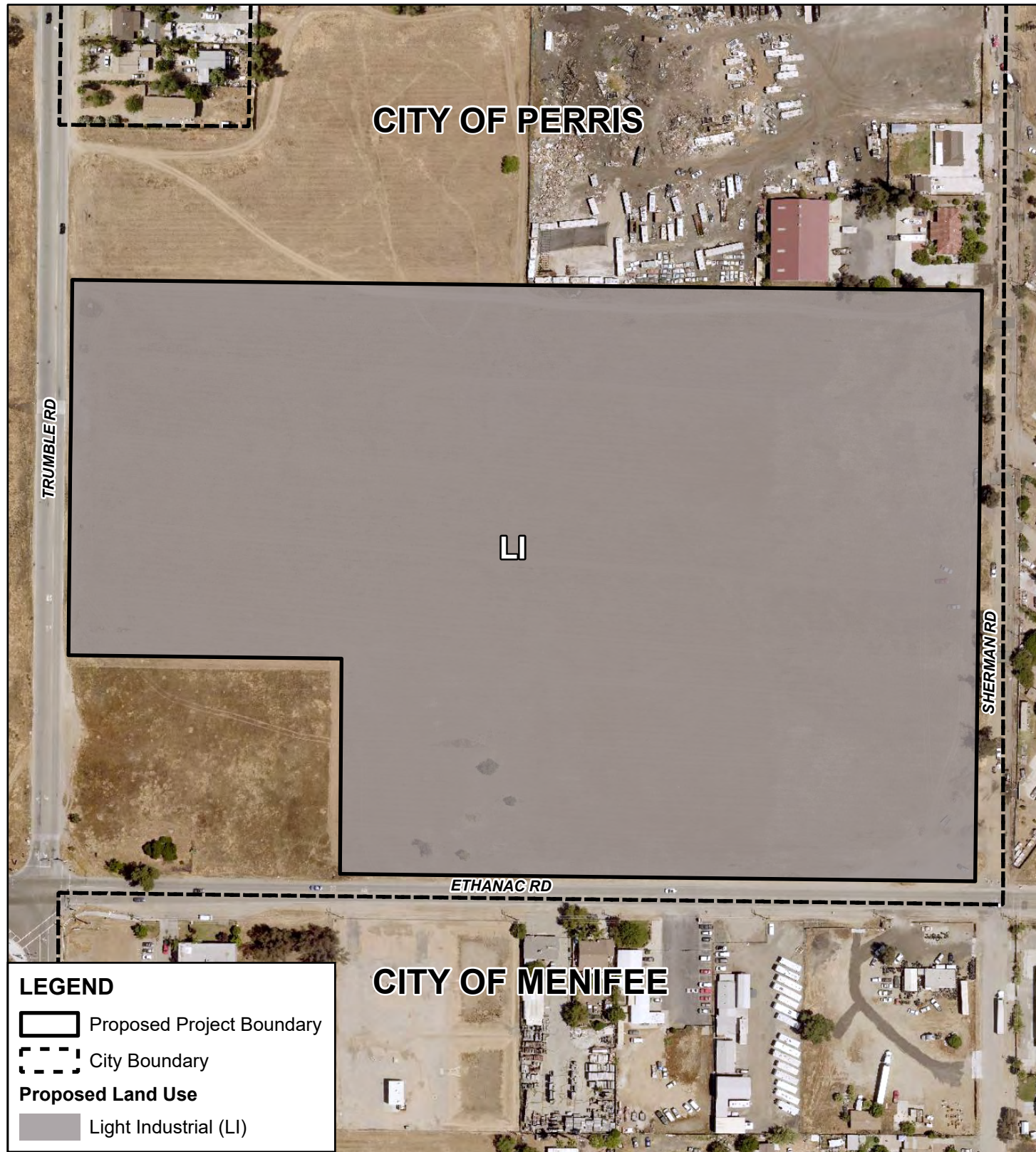
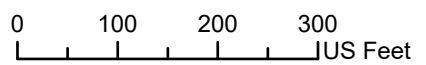
H:\2022\22-0198\GIS\Progen plan_change\gen_plan_change.aprx Map created 19 Oct 2022



LEGEND

- Project Boundary
- City Boundary
- Existing Land Use**
- Commercial Community (CC)

Source: Riverside County 2020; City of Perris, General Plan Land Use, 2018.



LEGEND

- Proposed Project Boundary
- City Boundary
- Proposed Land Use**
- Light Industrial (LI)

Proposed General Plan Land Use Amendment Hillwood Ethanac



NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. **Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org.** The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. For more information please contact **ALUC Planner Jackie Vega at (951) 955-0982.**

The City of Perris Planning Department should be contacted on non-ALUC issues. For more information, please contact City of Perris Planner Nathan Perez at 951-656-7000.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to javega@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please contact Barbara Santos at (951) 955-5132.

PLACE OF HEARING: **Riverside County Administration Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside California**

DATE OF HEARING: **June 8, 2023**

TIME OF HEARING: **9:30 A.M.**

CASE DESCRIPTION:

ZAP1570MA23 – Hillwood (Representative: Albert A. Webb & Associates)– City of Perris Case Nos. GPA22-0532 (General Plan Amendment), ZC22-05327 (Change of Zone), DPR22-00030 (Development Plan Review), TPM22-05328 (Tentative Parcel Map). A proposal to construct a 412,348 square foot manufacturing building with mezzanines on 20 acres, located northerly of Ethanac Road, westerly of Sherman Road, and easterly of Trumble Road. The applicant also proposes to amend the General Plan Land use designation and change the sites zoning from Commercial to Light Industrial. The applicant also proposes merging eight existing parcels into one. (Airport Compatibility Zone D of the March Air Reserve Base/Inland Port Airport Influence Area).



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC STAFF ONLY

ALUC Case Number: ZAP1570MA23

Date Submitted: 4/18/2023

AIA:

March



Zone: D



Public Hearing

Staff Review

Applicant

Applicant

Full Name: Hillwood c/o John Grace

Applicant Address: 901 Via Piedmonte, Suite 175 Ontario, CA 91764

Phone: (909) 256-5924

Email: John.Grace@Hillwood.com

Representative/ Property Owner Contact Information

Representative: Albert A. Webb Associates c/o Oscar Valadez

Email: Oscar.Valadez@webbassociates.com

Phone: (951) 295-9496

Address: 3788 McCray Street, Riverside CA, 92506

Property

Owner:

Hillwood c/o John Grace

Email: John.Grace@Hillwood.com

Phone: (909) 256-5924

Address: 901 Via Piedmonte, Suite 175 Ontario, CA 91764

Local Jurisdiction Agency

Agency

Name:

City of Perris Planning

Phone: (951) 943-5003 ext (279)

Staff Contact: Nathan Perez

Email: nperez@cityofperris.org

Address: 135 N. "D" Street Perris CA 92570-1906

Local Agency

Case No.:

DPR 22-00030 GPA22-0532, ZC22-05327 TPM 22-05328

Project Location

Street

North side of Ethanac Road between Trumble Road

Address: and Sherman Avenue in the City of Perris, California

Gross Parcel Size: 20±

Assessor's Parcel No.: 329-240-016 thru -020, -023 thru -027

Solar

Is the project proposing solar Panels? Yes

No

If yes, please provide solar glare study.
(only if in Zone C or higher)

Data

Site Elevation:(above mean sea level) 1433'

Height of Building or structures: 52'

What type of drainage basins are being proposed and the square footage: Contech underground chambers (56,900 s.f.) and a 35,000 Detention Basin

Notice

A. NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. REVIEW TIME: Estimated time for “staff level review” is approximately 30 days from date of submittal. Estimated time for “commission level review” is approximately 45 days from date of a complete application submittal to the next available commission hearing meeting.

C. SUBMISSION PACKAGE:

Please submit all application items DIGITALLY via USB or CD:

- Completed ALUC Application Form
- Plans Package: site plans, floor plans, building elevations, grading plans, subdivision maps
- Exhibits of change of zone, general plan amendment, specific plan amendment
- Project description of existing and proposed use

Additionally, please provide:

- ALUC fee payment (Checks made out to Riverside County ALUC)
- Gummed address labels of all surrounding property owners within a 300-foot radius of project site. (Only required if the project is scheduled for a public hearing).

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM: 3.2

HEARING DATE: June 8, 2023

CASE NUMBER: ZAP1569MA23 – Seven Multi-Site Solutions (Representative: EoS Fitness)

APPROVING JURISDICTION: City of Riverside

JURISDICTION CASE NO: PR-2023-001492 (Minor Conditional Use Permit)

LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

Airport Influence Area: March Air Reserve Base

Land Use Policy: Zone C2

Noise Levels: Below 60 CNEL contour

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends that the Commission find the Minor Conditional Use Permit **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, subject to the conditions included herein.

PROJECT DESCRIPTION: A proposal to establish a fitness center in an existing vacant 37,651 square foot tenant space, within an existing multi-tenant commercial shopping center on 9.35 acres.

PROJECT LOCATION: The site is located at 341 E. Alessandro Boulevard, westerly of Mission Grove Parkway, and northerly of Mission Village Drive, approximately 18,076 feet northwesterly of the northerly end of Runway 14-32 at March Air Reserve Base.

BACKGROUND:

Non-Residential Average-Acre Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zone C2, which limits average intensity to 200 people per acre.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan and the Additional Compatibility Policies included in the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, the following rates were used to calculate the occupancy for the proposed project:

- Exercise Rooms – 1 person per 50 square feet,
- Locker Room- 1 person per 50 square feet
- Retail area - 1 person per 115 square feet,
- Daycare area - 1 person per 30 square feet,
- Pool/Spa - 1 person per 50 square feet,
- Storage/equipment room - 1 person per 300 square feet, and
- Office – 1 person per 200 square feet.

The project proposes to construct a 37,651 square foot fitness center in an existing vacant tenant space, on a 9.35 acre parcel, which also contains an existing: 45,654 square foot grocery store, 1,200 square foot retail store, 1,125 square foot retail store, 1,625 square foot retail store, 3,200 square foot retail store, 2,365 square foot retail store, 1,800 square foot exercise center, 1,800 square foot restaurant, 2,320 square foot restaurant, 1,440 square foot office, 1,440 square foot retail store, 900 square foot retail store, and 2,100 square foot retail store, accommodating a total occupancy of 1,850 people, resulting in an average intensity of 198 people per acre, which is consistent with Zone C2 average intensity criterion of 200 people per acre.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle). Based on the number of parking spaces provided (504 standard vehicles), the total occupancy would be estimated at 756 people resulting in an average intensity of 81 people per acre, which is consistent with the Compatibility Zone C2 average intensity criterion of 200 people per acre.

Non-Residential Single-Acre Intensity: Compatibility Zone C2 limits maximum single-acre intensity to 500 people. There are no risk-reduction design bonuses available, as March Air Reserve Base/Inland Port Airport is primarily utilized by large aircraft weighing more than 12,500 pounds.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre area would include 21,813 square feet of exercise rooms, 101 square feet of office area, 440 square feet of reception area, 365 square feet of retail area, 1,332 square feet of daycare area, 1,486 square foot of pool/spa area, 557 square feet of mechanical equipment rooms, and 743 square feet of storage area, resulting in a single acre occupancy of 493 people which is consistent with the Compatibility Zone C2 single acre criterion of 500.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zones C2 (children's schools, day care centers, hospitals, nursing homes, libraries, places of assembly, highly noise-sensitive outdoor nonresidential uses and hazards to flight).

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being below the 60 CNEL range from aircraft noise. Therefore, no special measures are required to mitigate aircraft-generated noise.

Part 77: The elevation of Runway 14-32 at its northerly terminus is 1,535 feet above mean sea level. At a distance of approximately 18,076 feet from the runway to the site, Federal Aviation Administration (FAA) review would be required for any structures with top of roof elevation exceeding 1,716 feet AMSL. The site's finished floor elevation is 1,588 feet AMSL and the proposed building height is 35 feet, for a top point elevation of 1,623 feet AMSL. Therefore, review by the FAA

Obstruction Evaluation Service (FAA OES) was not required.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33C). The project is located 18,076 feet from the runway, and therefore would not be subject to the above requirement.

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight or circling climb following takeoff or toward an aircraft engaged in a straight or circling final approach toward a landing at an airport, other than a DoD or FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight or circling climb following takeoff or towards an aircraft engaged in a straight or circling final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Highly noise-sensitive outdoor nonresidential uses. Examples of noise-sensitive outdoor nonresidential uses that are prohibited include, but are not limited to, major spectator-oriented sports stadiums, amphitheaters, concert halls and drive-in theaters.

- (f) Other Hazards to flight.
3. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property, and be recorded as a deed notice.
 4. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

5. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
6. The project has been evaluated to establish a fitness center in an existing vacant 37,651 square foot tenant space. Any increase in building area, change in use to any higher intensity use, change in building location, or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.
7. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission and March Air Reserve Base.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS

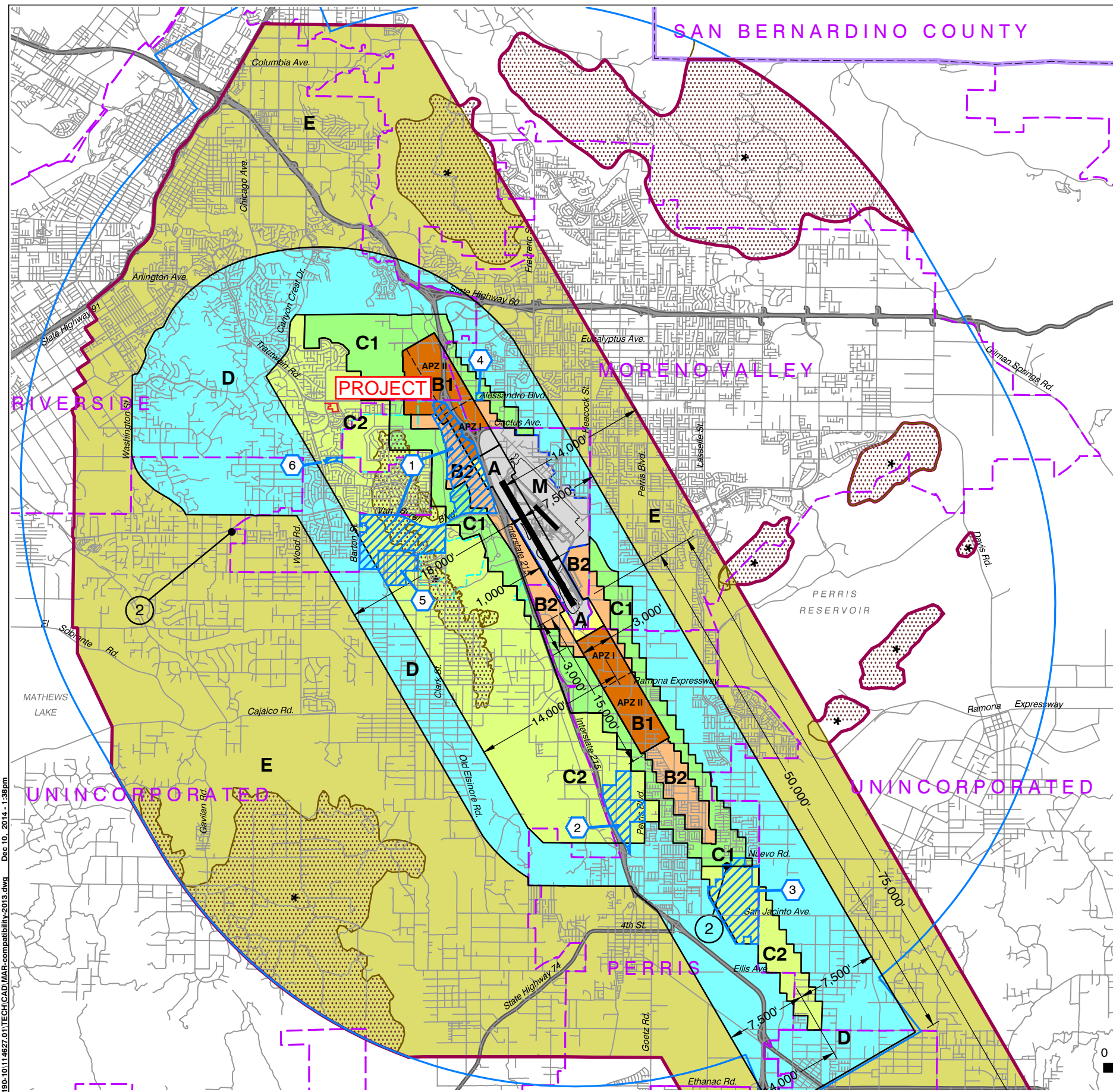
PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



LEGEND

Compatibility Zones

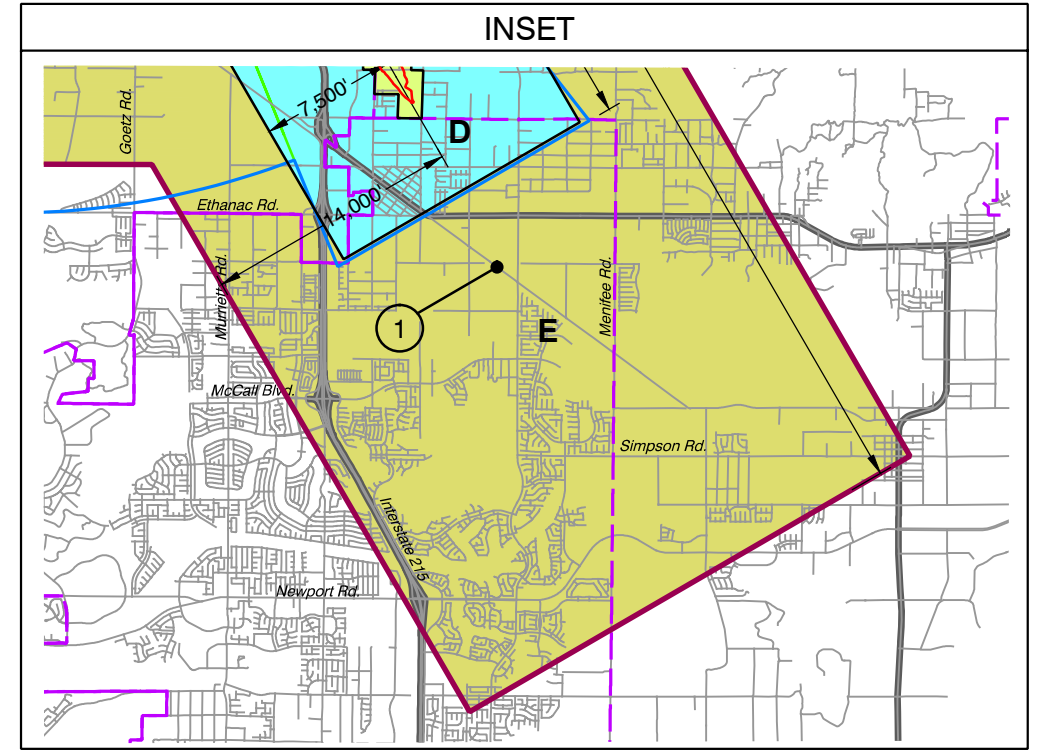
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

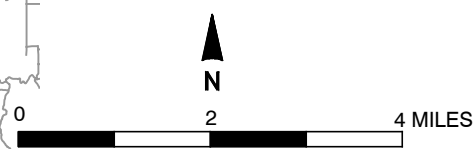
- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



**Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)**

Note:
All dimensions are measured from runway ends and centerlines.



Base map source: County of Riverside 2013

SEE INSET AT RIGHT

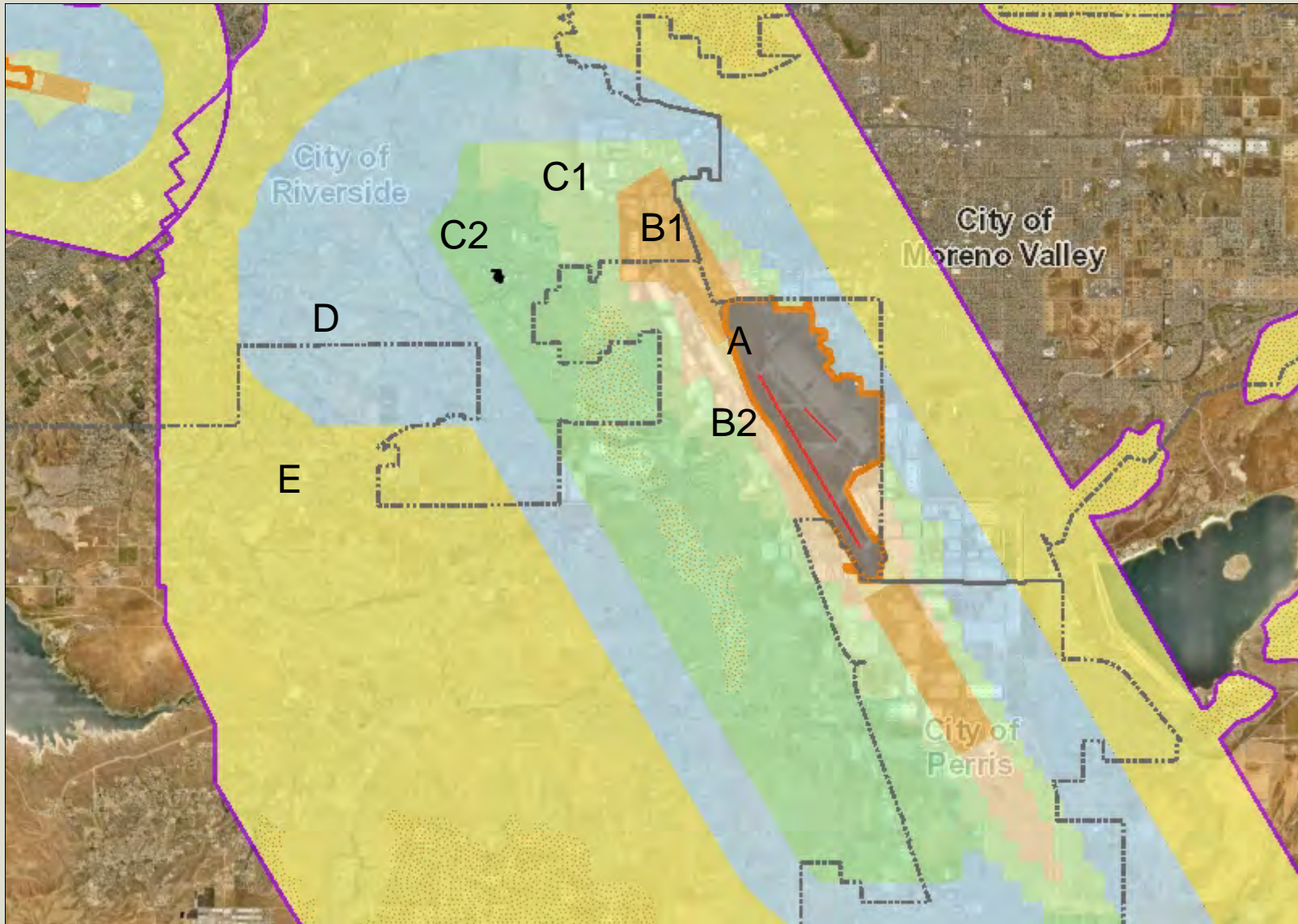
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Prepared by Mead & Hunt, Inc. (June 2013)

Map MA-1

**Compatibility Map
March Air Reserve Base / Inland Port Airport**

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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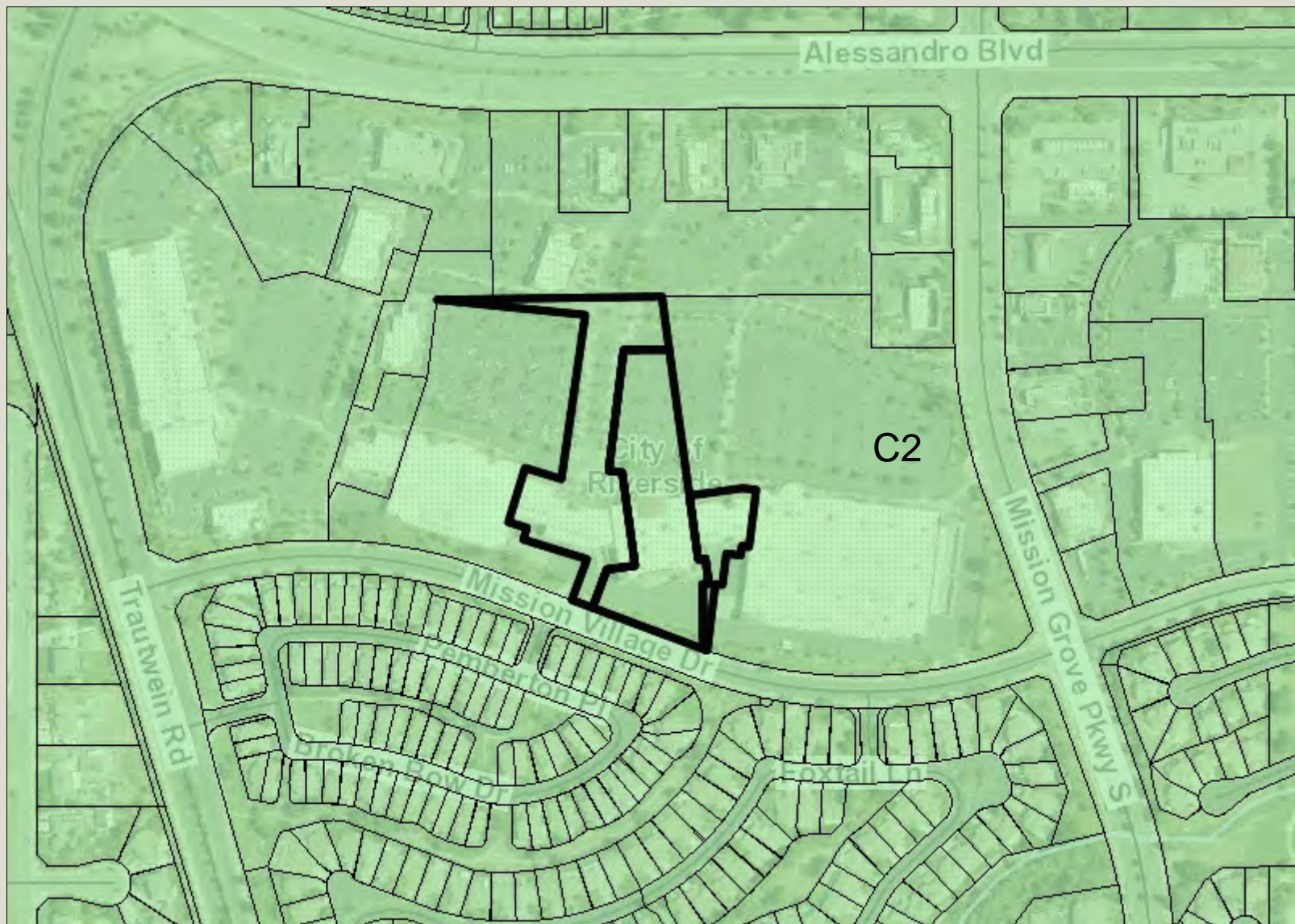


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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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Notes

Map My County Map



- Legend**
- Parcels
 - County Centerline Names
 - County Centerlines
 - Blueline Streams
 - City Areas
 - World Street Map



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Notes

Map My County Map



Legend

- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



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Notes



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Map My County Map



Legend

- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



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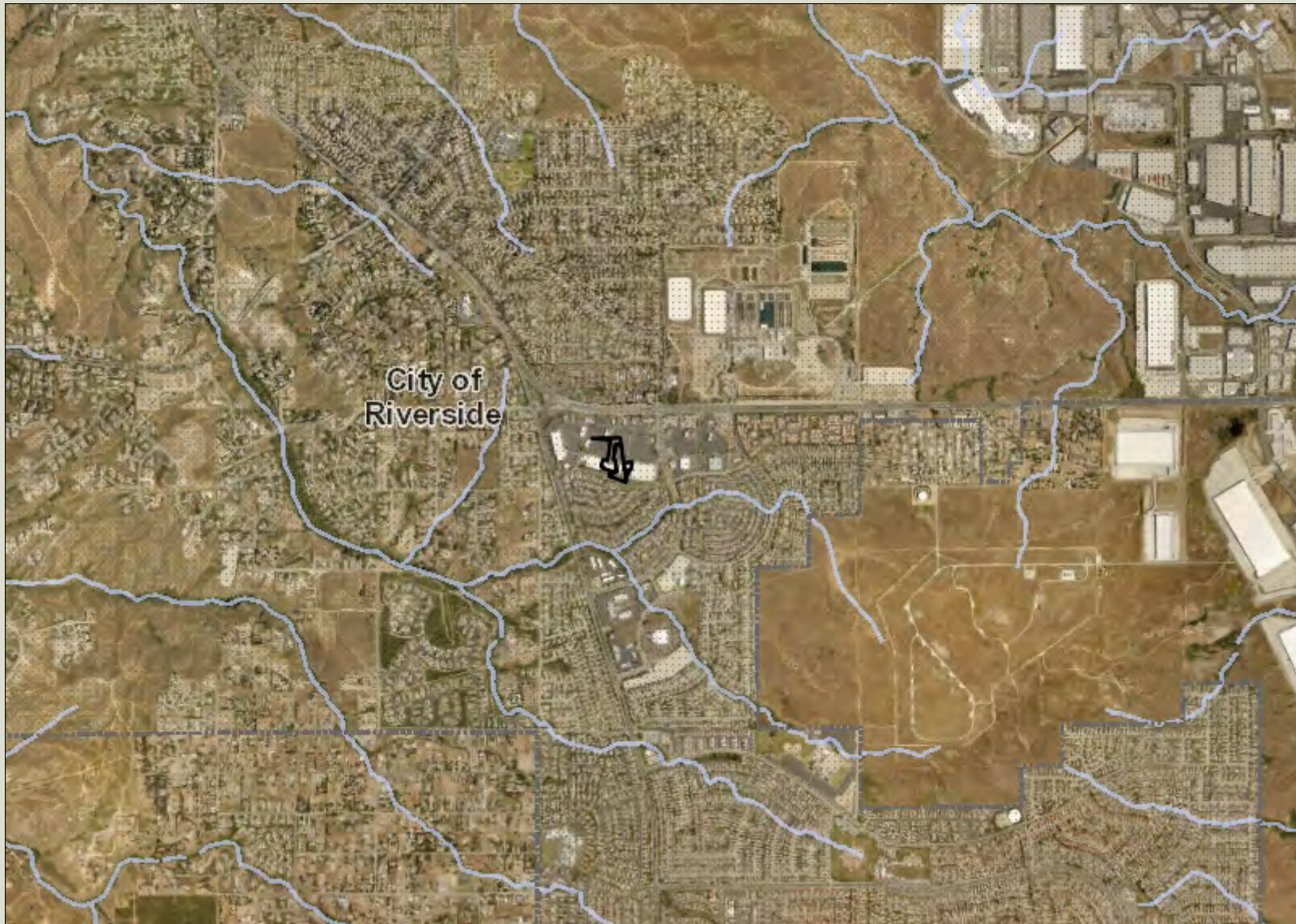
Notes

0 770 1,539 Feet


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Map My County Map



Legend

-  Blueline Streams
-  City Areas
-  World Street Map



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Notes

PRELIMINARY - NOT FOR CONSTRUCTION

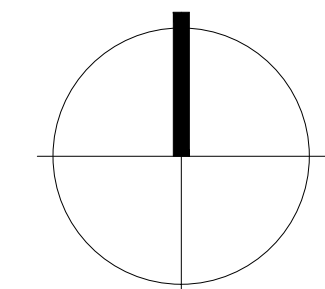
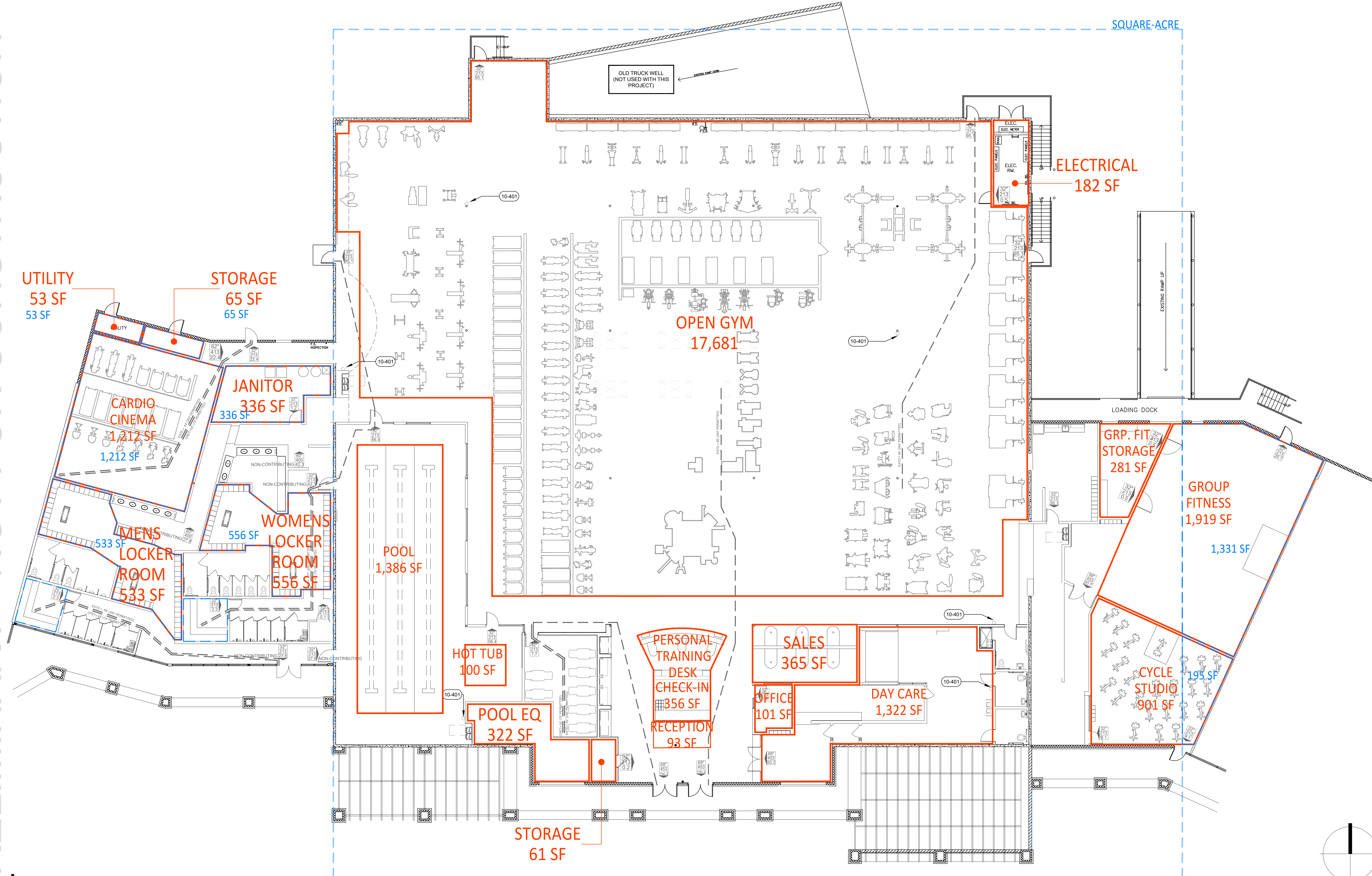
Occupant Load Tabulation						
Use	Total SF	SF Amount Removed Outside Square-Acre	Final Total SF	Occupant Load Factor	Total Occupants	Total Occupants within Square-Acre
Exercise Rooms	21,813	2,738	19,075	1/50	436	382
Offices	101	-	101	1/200	1	1
Reception/Lounge	449	-	449	1/15	30	30
Retail	365	-	365	1/115	3	3
Daycare	1,322	-	1,322	1/30	44	44
Locker Rooms	1,089	1,089	-	1/50	22	-
Pool/Spa	1,486	-	1,486	1/50	30	30
Mechanical Equipment Rooms	557	53	504	1/300	2	2
Storage	743	401	342	1/300	2	1
Total					570	493

EXIT PLAN NOTES & LEGEND

- COMPLY WITH ALL REQUIREMENTS OF THE 2022 CALIFORNIA BUILDING CODE (CBC)
- EATD = EXIT ACCESS TRAVEL DISTANCE PER TABLE 1016.1
- CPET = COMMON PATH OF EGRESS TRAVEL PER CBC 1014.3
- EXIT SIGNS SHALL BE PROVIDED PER CBC SECTION 1011
- OCCUPANT LOAD SIGNS SHALL BE PROVIDED IN ALL ASSEMBLY ROOMS IN ACCORDANCE WITH 1004.3 IN THE FOLLOWING ROOMS:
 - OPEN GYM
 - KIDS CLUB
 - CARDIO CINEMA
 - GROUP FITNESS
- AUTOMATIC SPRINKLER SYSTEM SHALL BE PROVIDED IN ACCORDANCE WITH CBC SECTION 903 AND CFC
- FIRE ALARM SYSTEM SHALL BE PROVIDED IN ACCORDANCE WITH CBC SECTION 907 AND CFC
- PORTABLE FIRE EXTINGUISHERS SHALL BE PROVIDED IN ACCORDANCE WITH CBC 906.1 AND CFC - 75' MAXIMUM TRAVEL DISTANCE TO EXTINGUISHER
- | | |
|---|--|
| X | EXIT WIDTH PROVIDED (32" CLEAR EXIT WIDTH USED FOR 36" DOOR LEAFS) |
| X | MAXIMUM NUMBER OF OCCUPANTS THIS EXIT CAN ACCOMMODATE |
| X | NUMBER OF OCCUPANTS UTILIZING THIS EXIT |
- EXIT SIGN
- FIRE EXTINGUISHER

EXIT PLAN KEYNOTES

- 10-401 FIRE EXTINGUISHER MOUNTED ON BRACKET PER CBC 906.9.1 AND CFC. PROVIDE FIRE EXTINGUISHER SIGNAGE WHERE VISUAL OBSTRUCTIONS MAY EXIST



ALUC EXITING PLAN
SCALE: 1" = 10'-0"



Stamp
Date: 4-19-23
Revision: 5-15-23

Eos FITNESS
RIVERSIDE
335 Alessandro Blvd. (CARTA)
Riverside, CA 92508
for Eos Fitness Holdings, LLC

Project #: 22118
Drawn by: QO

ALUC EXITING PLAN

G0.4a



WWW.EVOGENESYS.COM



PROJECT DESCRIPTION:

THIS FRONT FACADE IS APPROXIMATELY 225 FT. LONG OF A RETAIL CENTER, ONE STORIED STRUCTURE WITH APPROXIMATELY 22 FT ROOF HEIGHT. GROUND LEVEL WITH APPROXIMATELY 771 FEET PERIMETER AND EXTERIOR WALLS ARE ANTICIPATED TO BE 8" THICK AND INTERIOR DEMISING WALLS ARE METAL STUD FRAME PARTITIONS WALLS, STRUCTURAL SYSTEM IS COMPOSED OF CONCRET COLUMNS AND EXTERIOR WOOD FRAMING.

335 ALESSANDRO BLVD

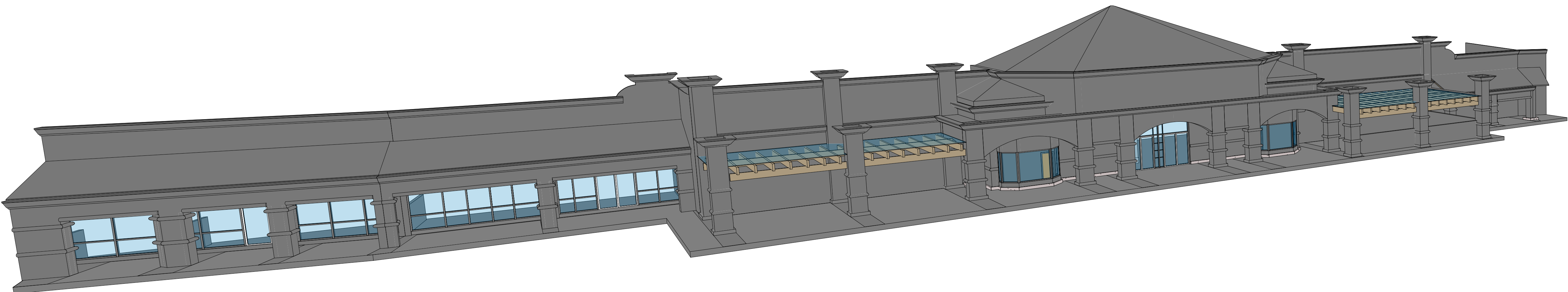
EVOGENESYS LLC

RIVERSIDE, CALIFORNIA
CA 92508, USA.



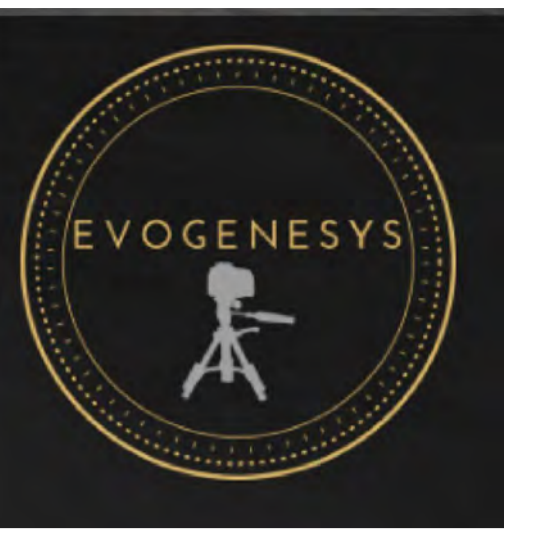
2 3D - OVERALL

No.	Description	Date



COVER SHEET

Project Number	Project Number
Date	Issue Date
Drawn By	Author
Checked By	Checker
100	
Scale	

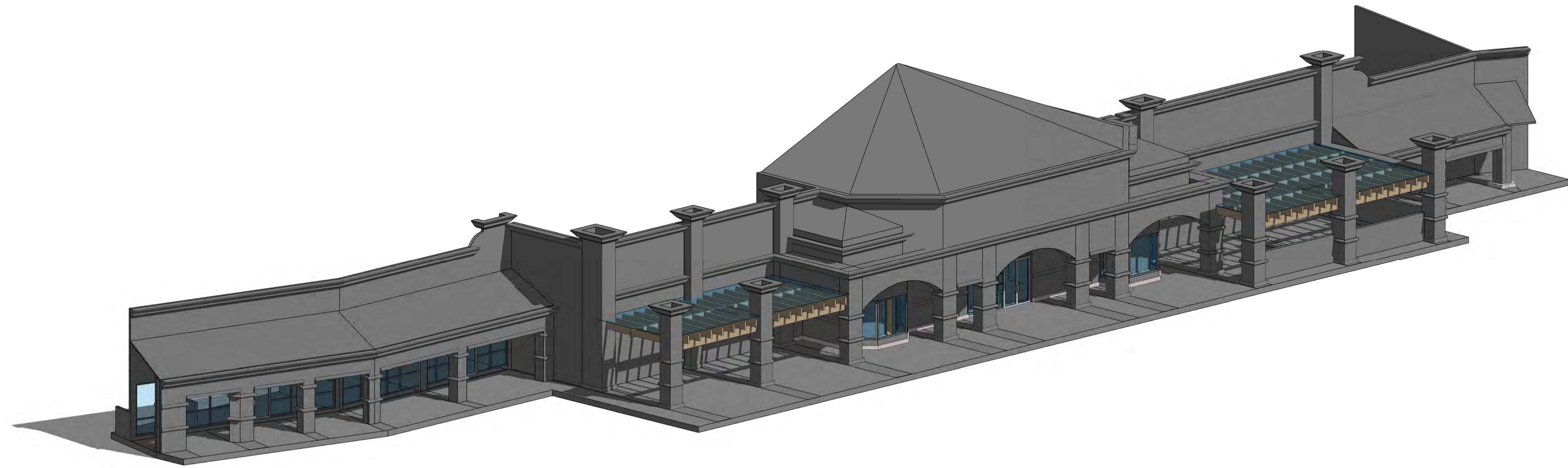


WWW.EVOGENESYS.COM

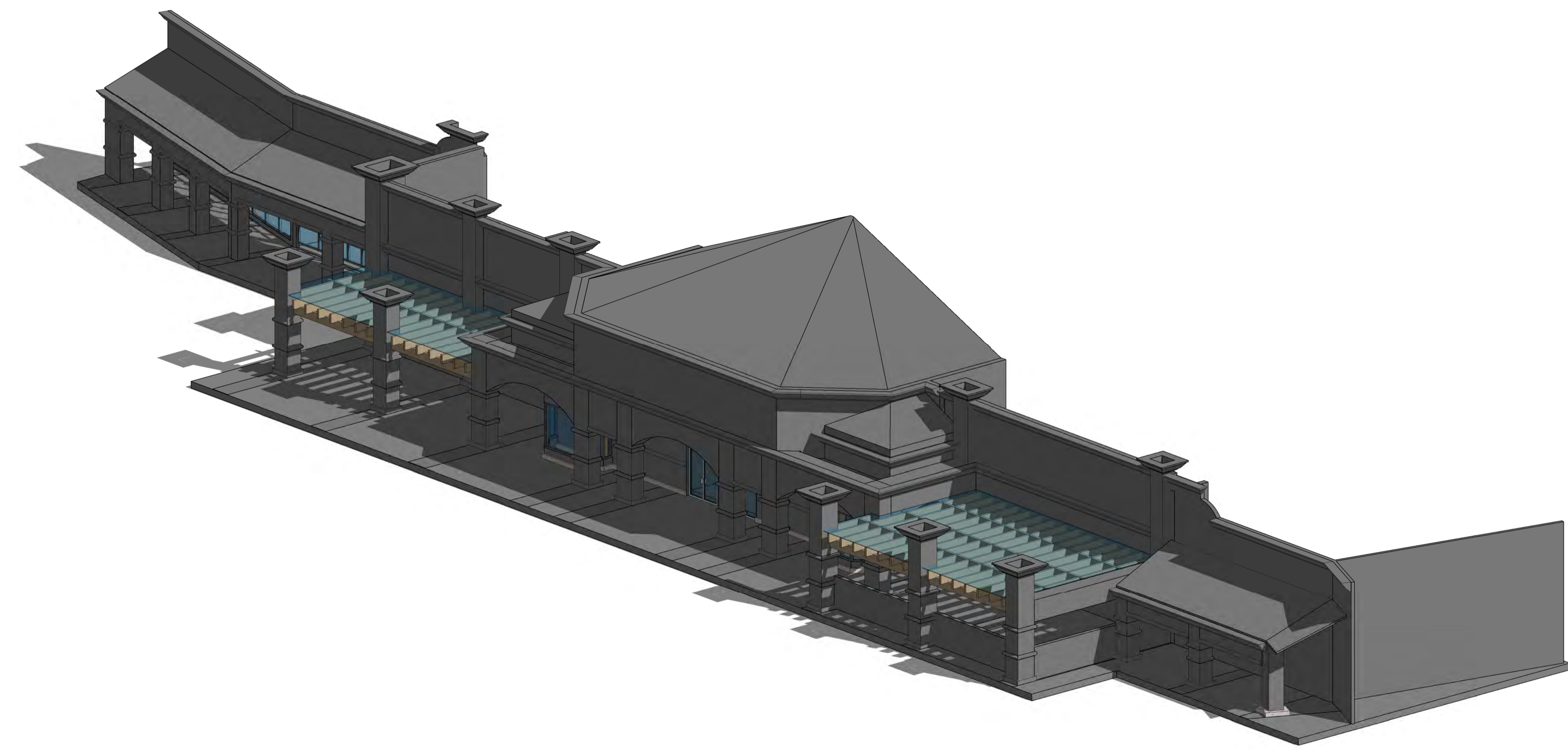
335 ALESSANDRO BLVD

EVOGENESYS LLC

RIVERSIDE, CALIFORNIA
CA 92508, USA.



① 3D VIEW - SW FRONT ELEVATION



② 3D VIEW - SE FRONT ELEVATION

No.	Description	Date

3D VIEWS EXISTING STORE FRONT

Project Number	Project Number
Date	Issue Date
Drawn By	Author
Checked By	Checker

101

Scale

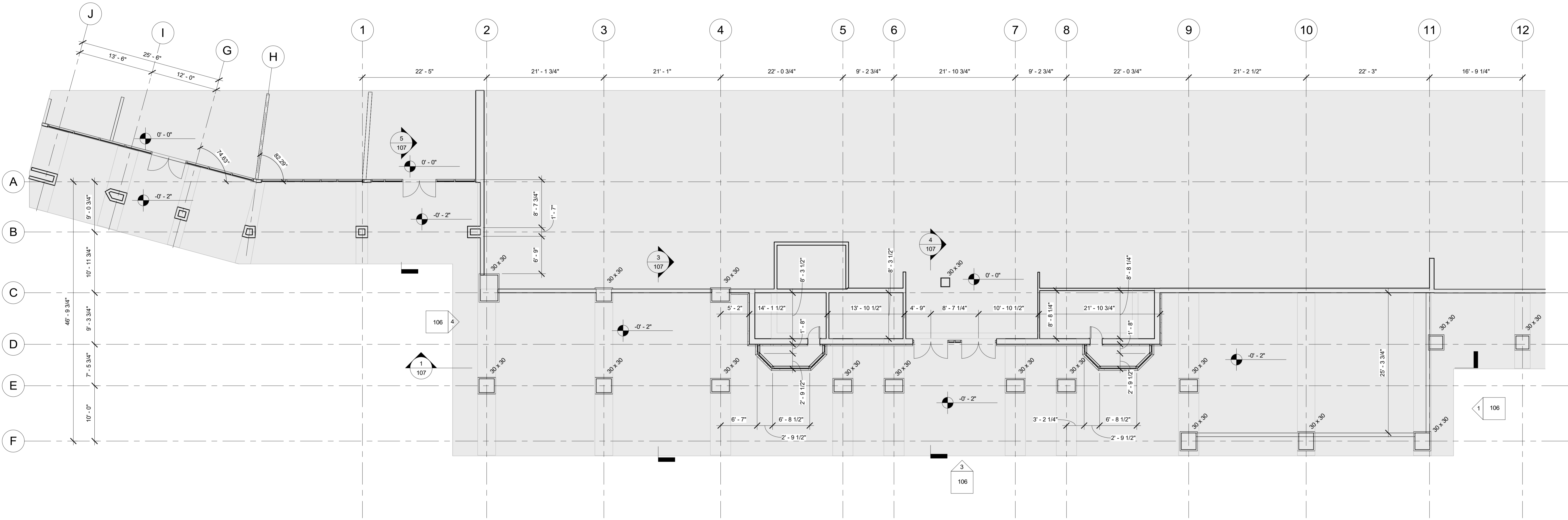


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335 ALESSANDRO BLVD

EVOGENESYS LLC

RIVERSIDE, CALIFORNIA
CA 92508, USA.



1 Ground Floor Plan
1/8" = 1'-0"

No.	Description	Date

EXISTING CONDITIONS FLOOR PLAN

Project Number	Project Number
Date	Issue Date
Drawn By	E.C.
Checked By	Checker

103

Scale 1/8" = 1'-0"

5/16/2023 2:47:00 PM

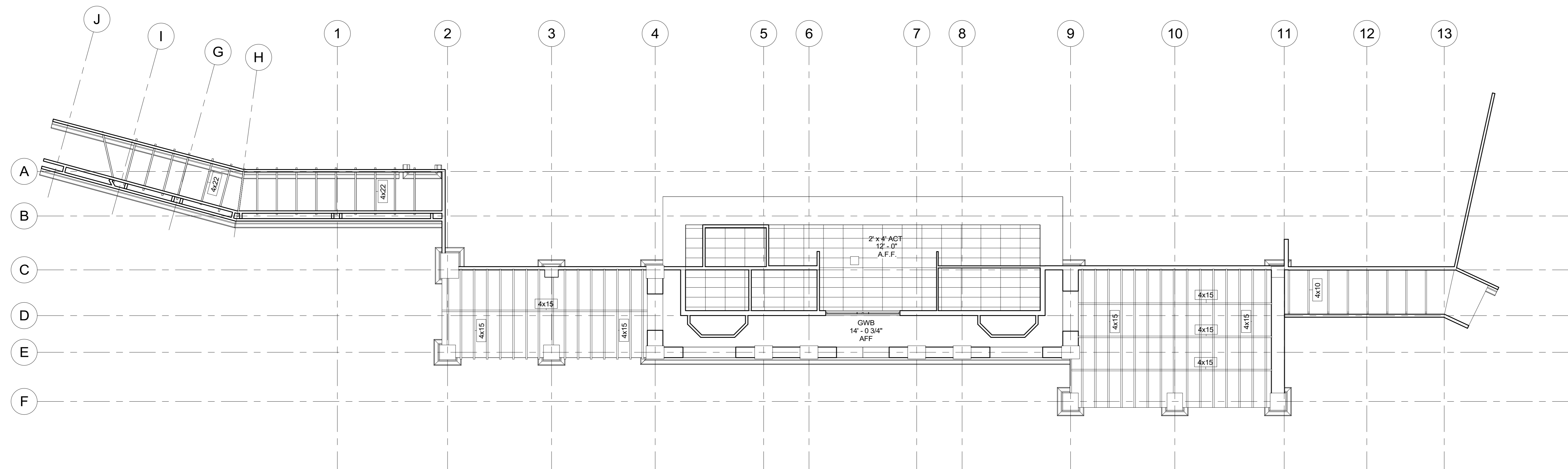


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335 ALESSANDRO BLVD

EVOGENESYS LLC

RIVERSIDE, CALIFORNIA
CA 92508, USA



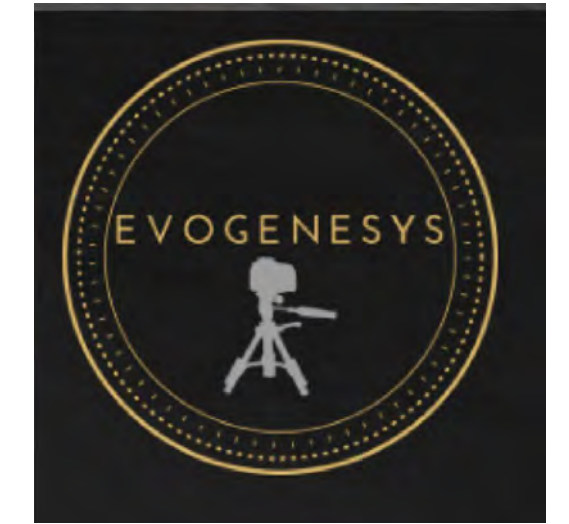
1 Ground Floor - RCP
3/32" = 1'-0"

No.	Description	Date

EXISTING
CONDITIONS
FLOOR - RCP

Project Number	Project Number
Date	Issue Date
Drawn By	Author
Checked By	Checker
104	
Scale	3/32" = 1'-0"

5/16/2023 2:47:00 PM

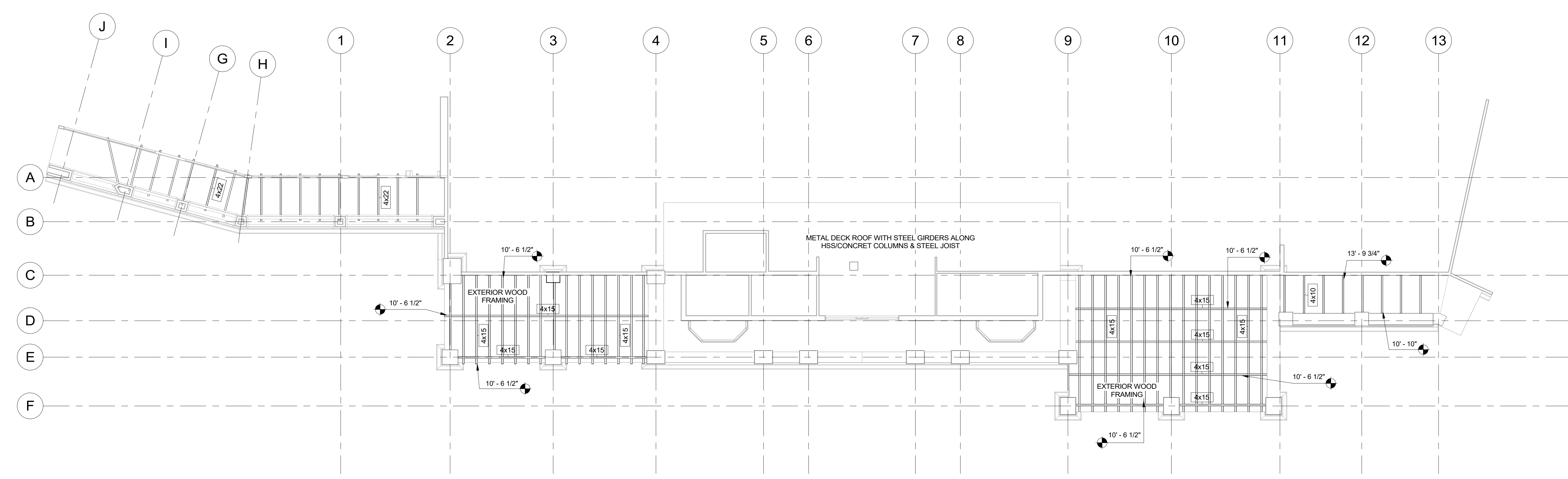


WWW.EVOGENESYS.COM

335 ALESSANDRO BLVD

EVOGENESYS LLC

RIVERSIDE, CALIFORNIA
CA 92508, USA



1 Ground Floor - Framing Plan
3/32" = 1'-0"

No.	Description	Date

EXISTING
CONDITIONS
FRAMING PLAN

Project Number	Project Number
Date	Issue Date
Drawn By	Author
Checked By	Checker

105

Scale 3/32" = 1'-0"

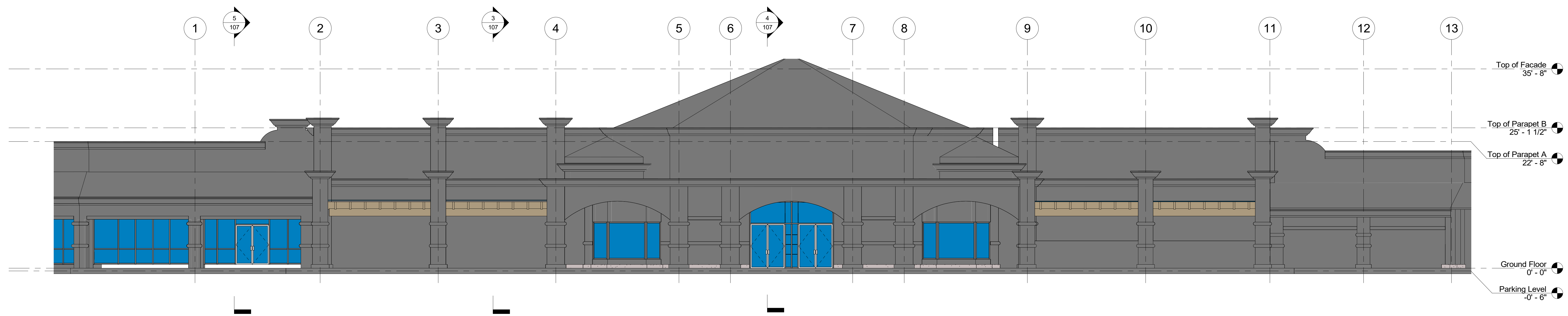


WWW.EVOGENESYS.COM

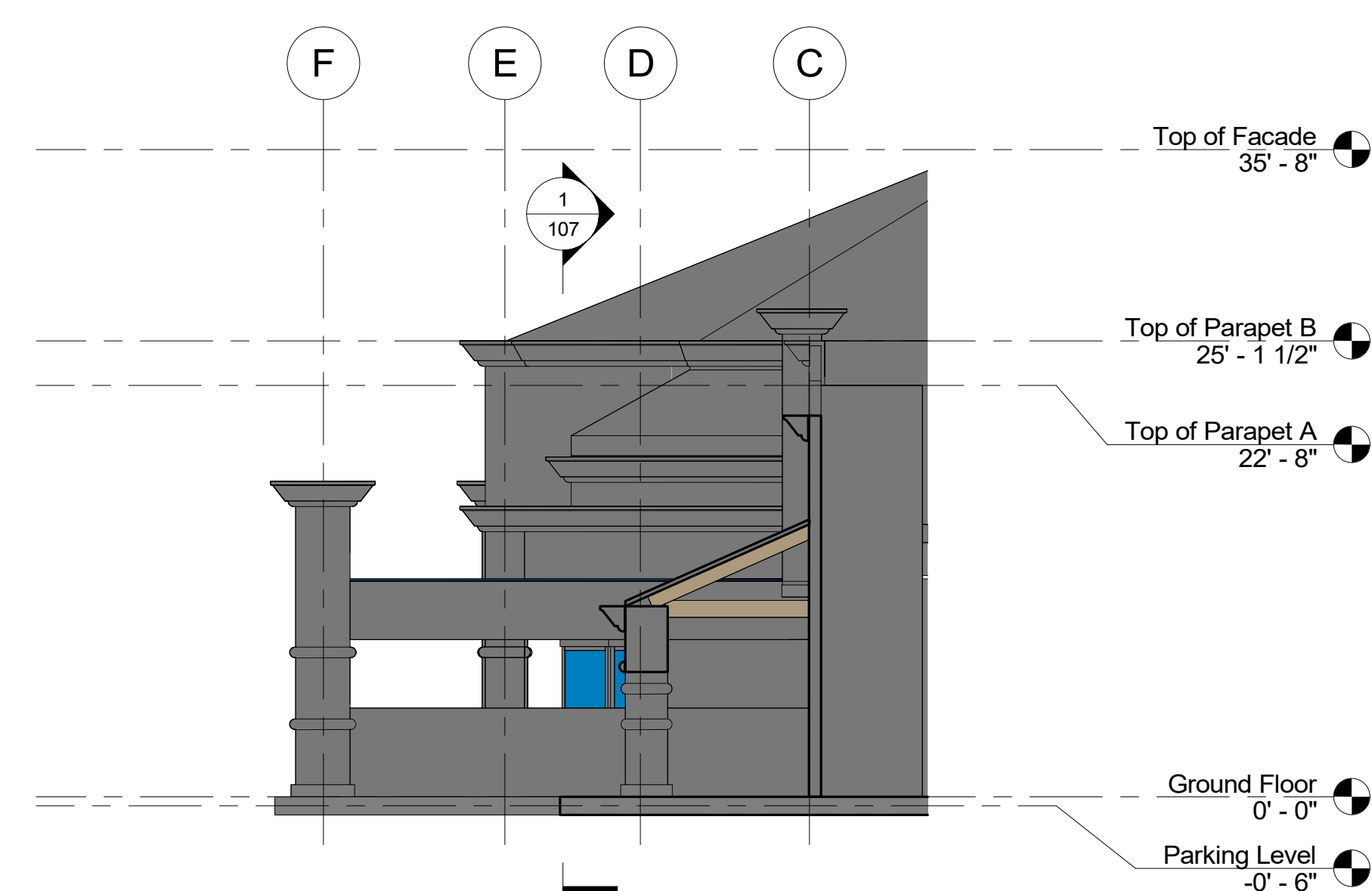
335 ALESSANDRO BLVD

EVOGENESYS LLC

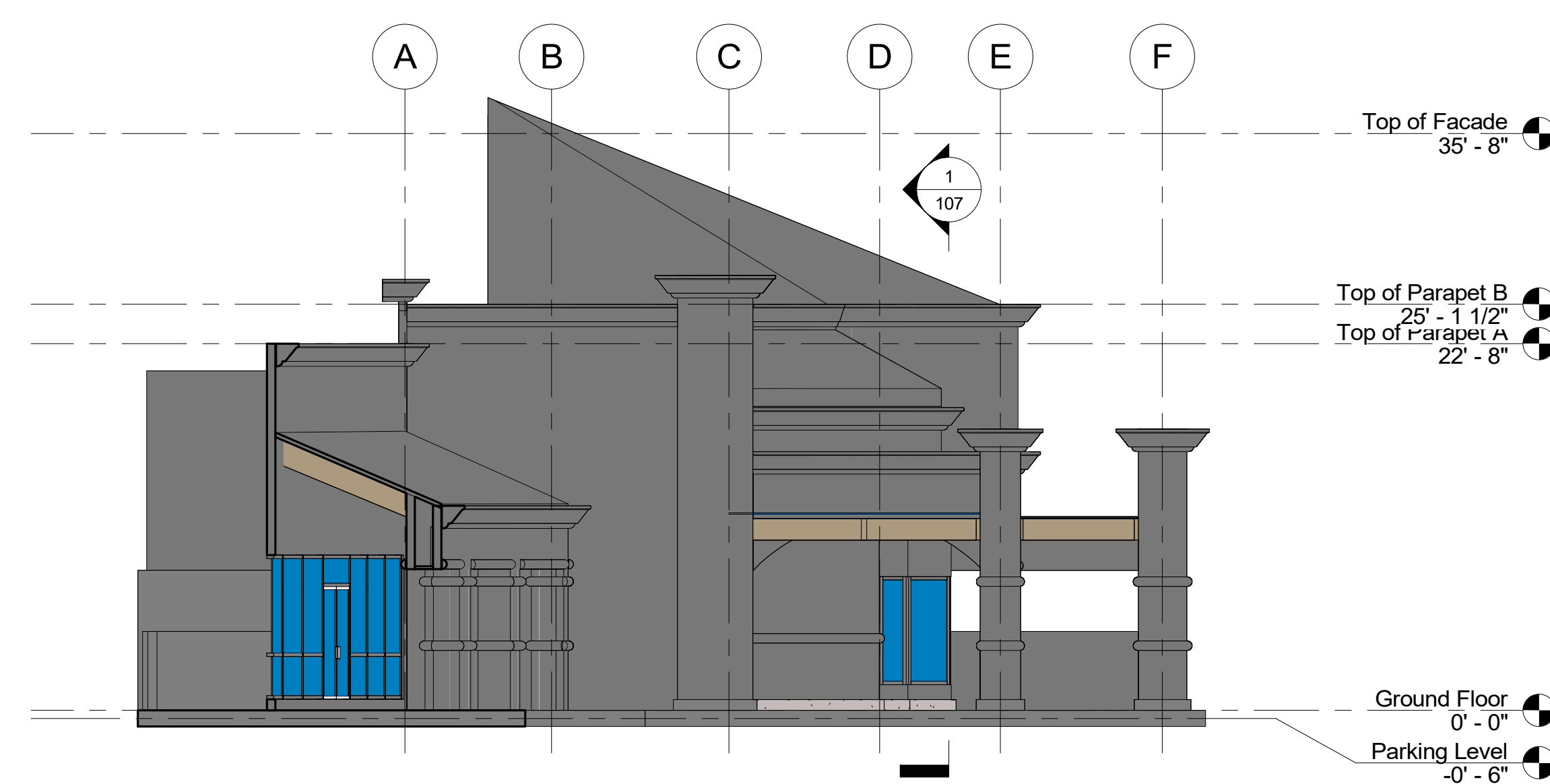
RIVERSIDE, CALIFORNIA
CA 92508, USA.



3 South Elevation
1/8" = 1'-0"



1 East Elevation
1/8" = 1'-0"



4 West Elevation
1/8" = 1'-0"

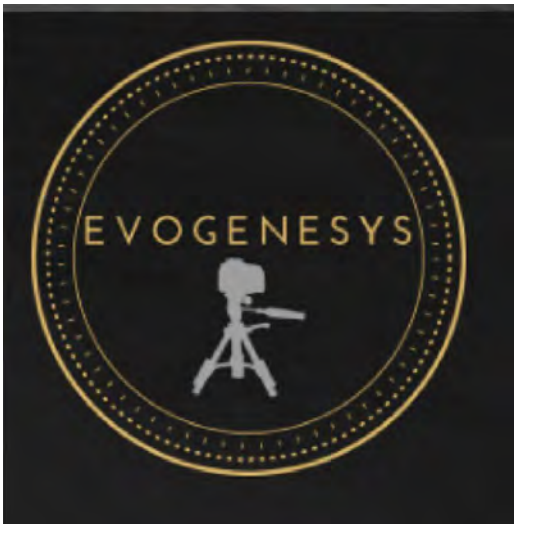
No.	Description	Date

EXISTING EXTERIOR ELEVATIONS

Project Number	Project Number
Date	Issue Date
Drawn By	Author
Checked By	Checker

106

Scale	1/8" = 1'-0"
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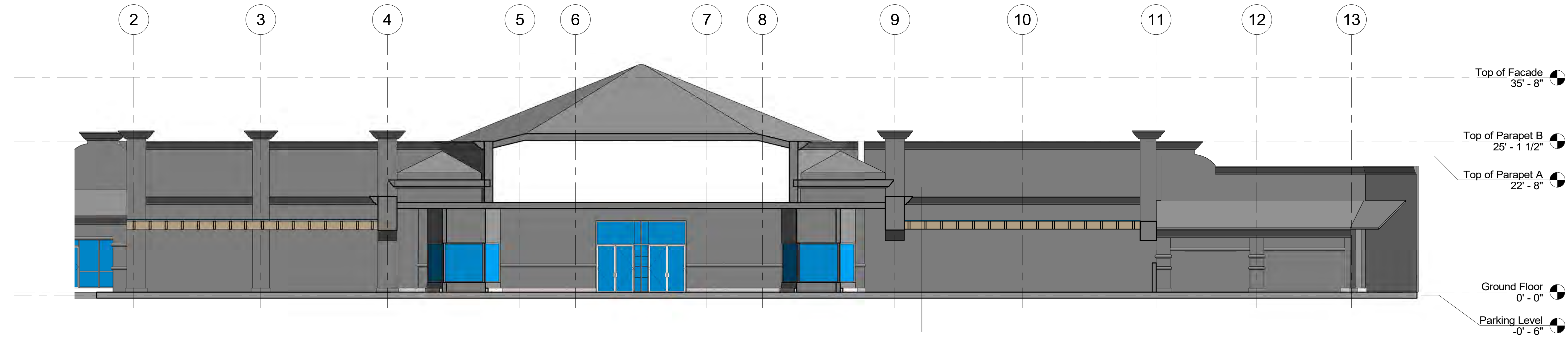


WWW.EVOGENESYS.COM

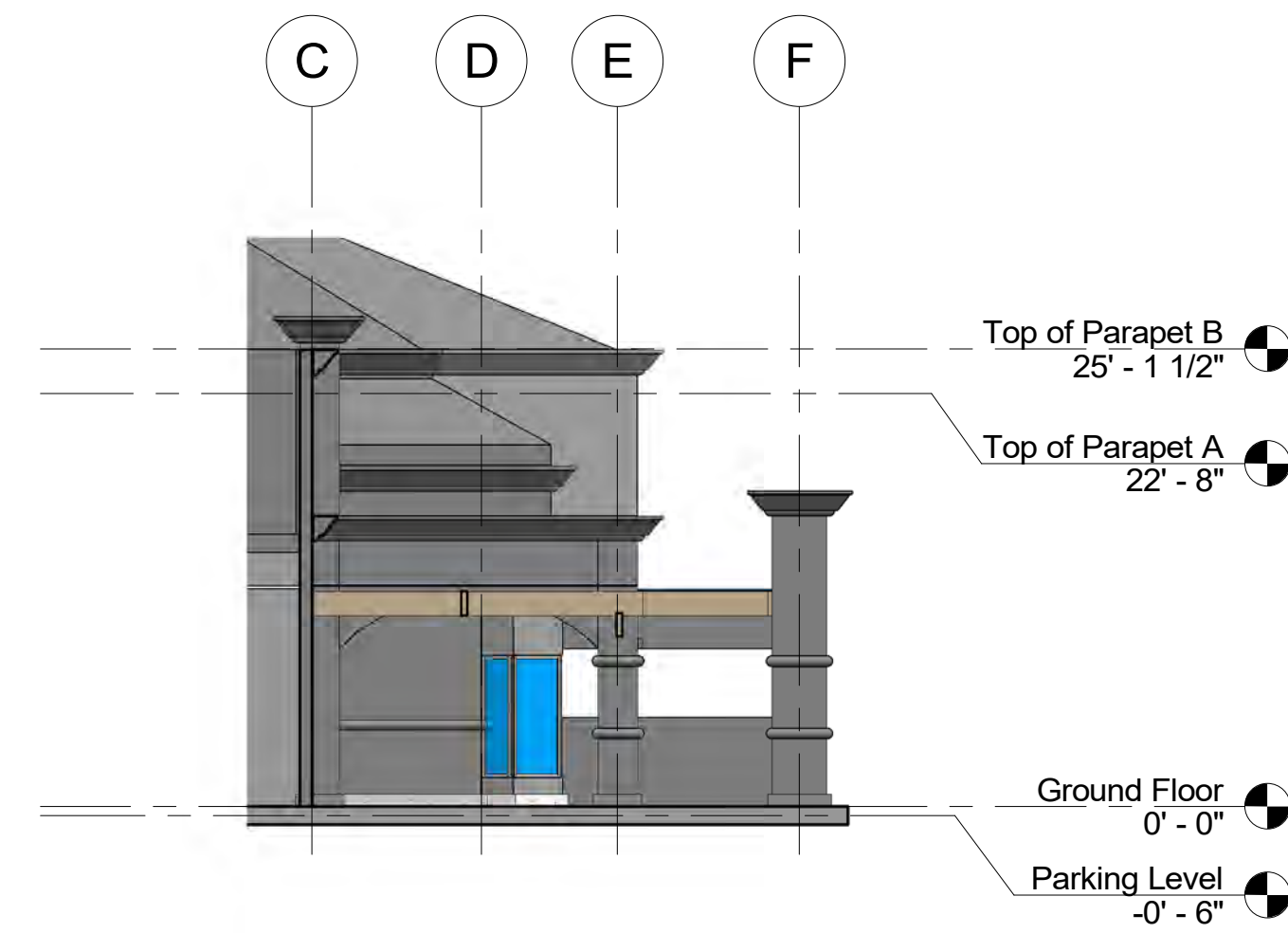
335 ALESSANDRO BLVD

EVOGENESYS LLC

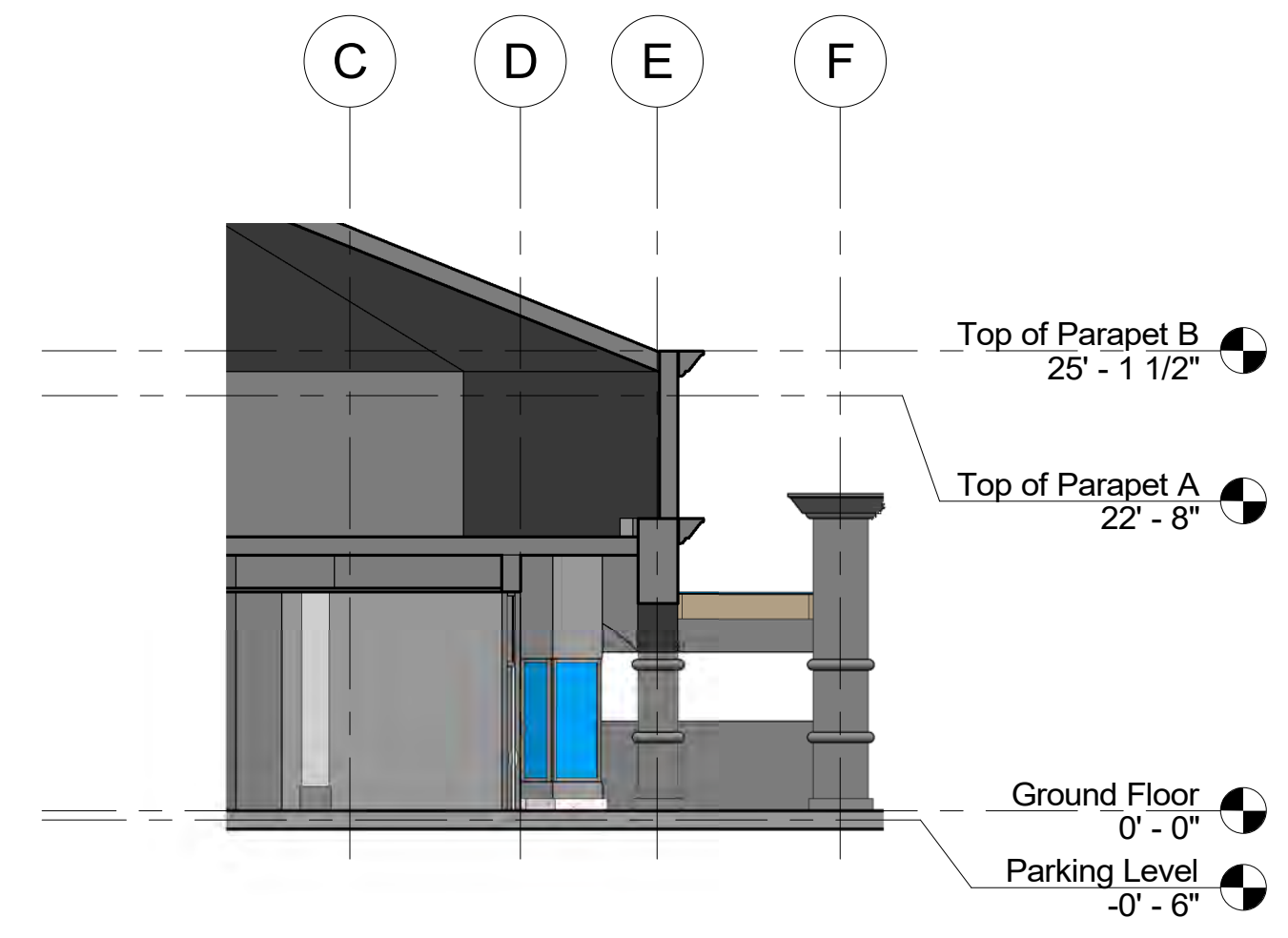
RIVERSIDE, CALIFORNIA
CA 92508, USA



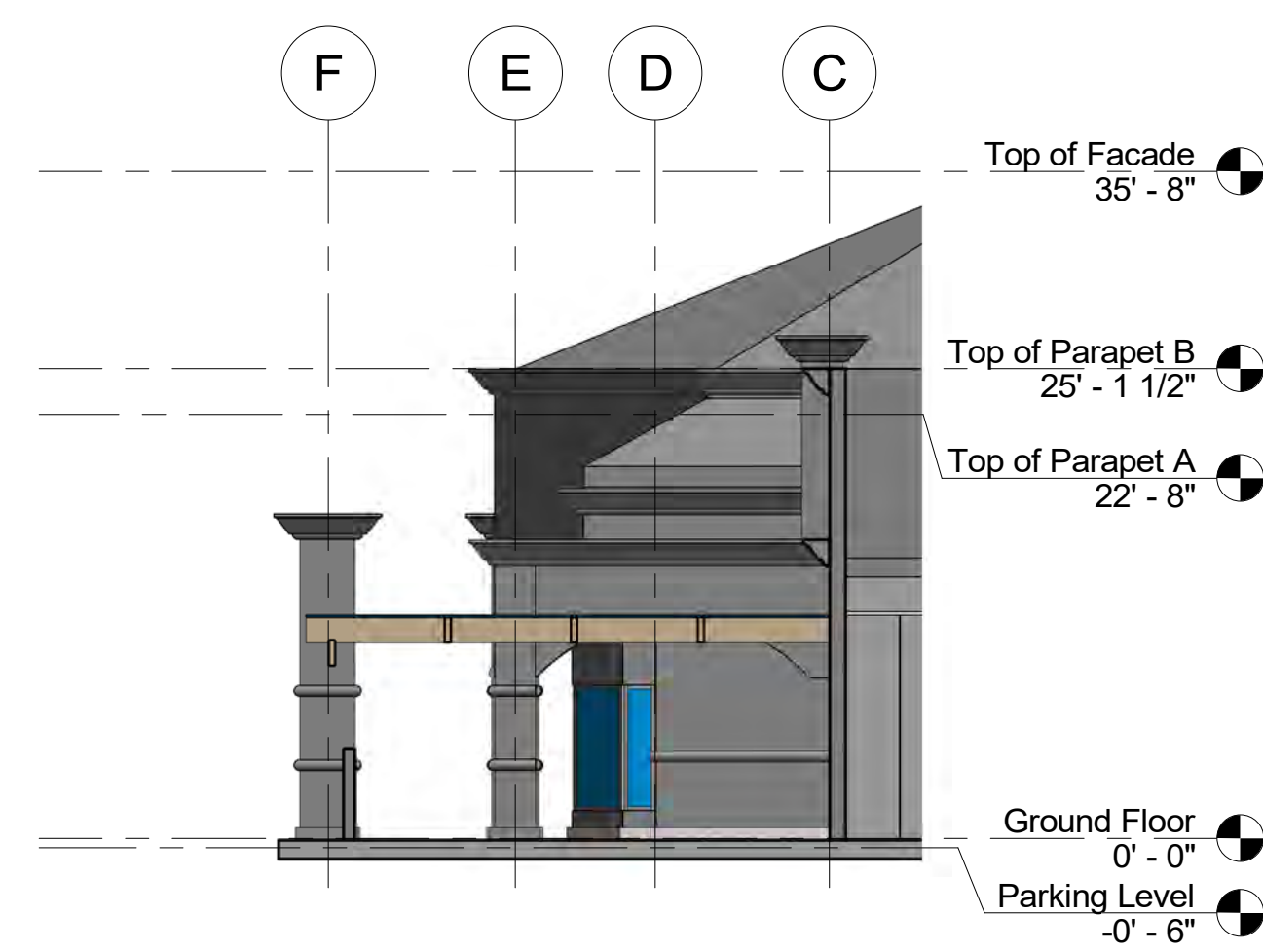
1 Longitudinal Section
1" = 10'-0"



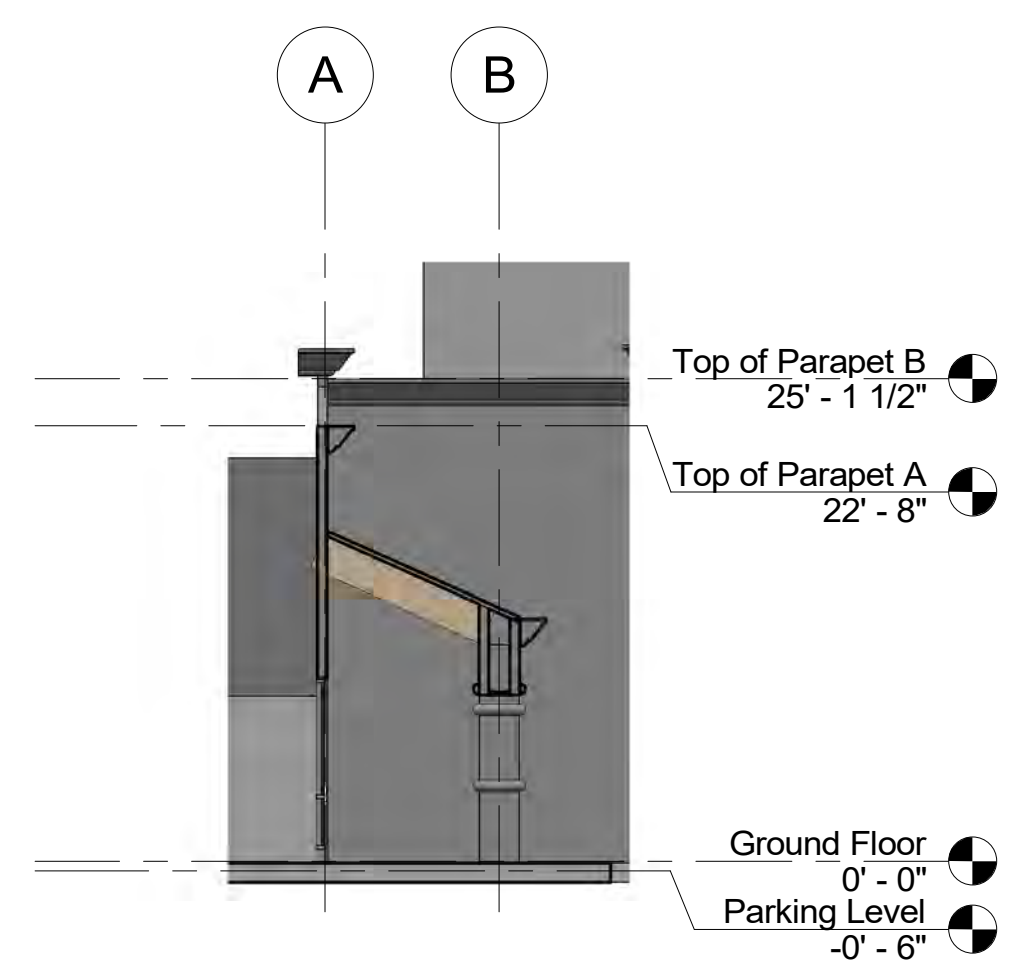
3 Section A
1" = 10'-0"



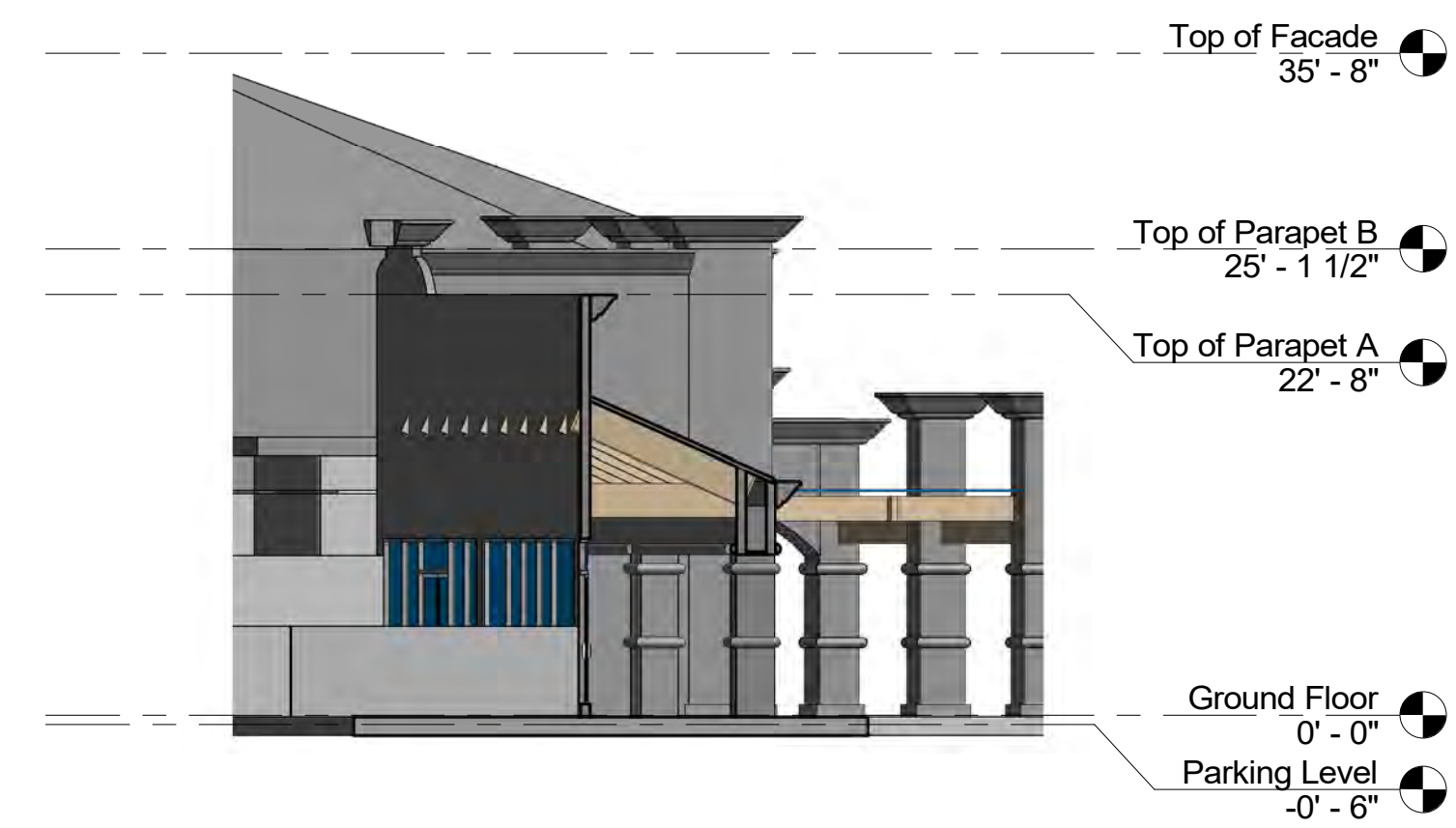
4 Section B
1" = 10'-0"



2 Section C
1" = 10'-0"



5 Section D
1" = 10'-0"



6 Section E
1" = 10'-0"

No.	Description	Date

3D - EXISTING SECTION VIEWS

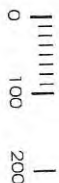
Project Number	Project Number
Date	Issue Date
Drawn By	Author
Checked By	Checker
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Scale	1" = 10'-0"



REGIONAL PROPERTIES, INC.

MISSION GROVE PLAZA - RIVERSIDE, CA

ILLUSTRATIVE SITE PLAN



01/06/17



ARCHITECTS
OAKLAND



NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. **Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org.** The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. For more information please contact **ALUC Planner Jackie Vega at (951) 955-0982.**

The City of Riverside Planning Department should be contacted on non-ALUC issues. For more information please contact City of Riverside Planner Judy Eguez at (951) 826-3969.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to javega@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please contact Barbara Santos at (951) 955-5132.

PLACE OF HEARING: **Riverside County Administration Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside California**

DATE OF HEARING: **June 8, 2023**

TIME OF HEARING: **9:30 A.M.**

CASE DESCRIPTION:

ZAP1569MA23 – Seven Multi-Site Solutions (Representative: EoS Fitness) – City of Riverside Case No. PR-2023-001492 (Minor Conditional Use Permit). A proposal to establish a fitness center in an existing vacant 37,651 square foot tenant space, within an existing multi-tenant commercial shopping center on 9.35 acres, located at 341 E. Alessandro Boulevard, westerly of Mission Grove Parkway, and northerly of Mission Village Drive (Airport Compatibility Zone C2 of the March Air Reserve Base/Inland Port Airport Influence Area).



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC STAFF ONLY

ALUC Case Number: ZAP1569MA23

Date Submitted: 4/6/23

AIA: March

Zone: C2

Public Hearing

Staff Review

Applicant

Applicant Full Name: Joseph Bermudez - Sevan Multi-Site Solutions

Applicant Address: 3025 Highland Pkwy Suite 850

Phone: 253-508-3104

Email: joseph.bermudez@sevansolutions.com

Representative/ Property Owner Contact Information

Representative: Arthur Babcock - EoS Fitness

Email: ababcock@eosfitness.com

Phone: 661-529-5766

Address: 3200 N. Central Ave., Phoenix, AZ, 85012

Property Owner: Regional Properties, Inc.

Email: michelle@regionalpropertiesinc.com

Michelle Rubin

Phone: 310-553-7846

Address: 9150 Wilshire Blvd., Suite 210, Beverly Hills, CA 90212

Local Jurisdiction Agency

Agency Name: City of Riverside - Community & Economic Development, Planning Division

Phone: 951.826.3969

Staff Contact: Judy Equez

Email: JEquez@riversideca.gov

Address: 3900 Main Street, 3rd Floor, Riverside, CA 92522

Local Agency Case No.: DP-2022-01641

Project Location

Street Address: 321, 335, & 341 Alessandro Blvd. Gross Parcel Size: 4.7 ac

Assessor's Parcel No.: 276-110-015, 276-110-016, 276-110-017

Solar

Is the project proposing solar Panels? Yes

No

If yes, please provide solar glare study.
(Only for zone C or higher.)

Data

Site Elevation:(above
mean sea level) _____

Height of Building or
structures: _____

What type of drainage basins are
being proposed and the square
footage: _____

Notice

A. NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. REVIEW TIME: Estimated time for “staff level review” is approximately 30 days from date of submittal. Estimated time for “commission level review” is approximately 45 days from date of a complete application submittal to the next available commission hearing meeting.

C. SUBMISSION PACKAGE:

Please submit all application items DIGITALLY via USB or CD:

- Completed ALUC Application Form
- Plans Package: site plans, floor plans, building elevations, grading plans, subdivision maps
- Exhibits of change of zone, general plan amendment, specific plan amendment
- Project description of current and proposed use

Additionally, please provide:

- ALUC fee payment (Checks made out to Riverside County ALUC)
- Gummed address labels of all surrounding property owners within a 300-foot radius of project site. (Only required if the project is scheduled for a public hearing)

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM: 3.3

HEARING DATE: June 8, 2023

CASE NUMBER: ZAP1125FV23 – Wallace Design Group (Representative: West Coast Self Storage)

APPROVING JURISDICTION: County of Riverside

JURISDICTION CASE NO: PP20682R01 (Plot Plan Revision)

LAND USE PLAN: 2007 French Valley Airport Land Use Compatibility Plan, as amended in 2011

Airport Influence Area: French Valley Airport

Land Use Policy: Zones C

Noise Levels: Below 55 CNEL from aircraft

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends that the Commission find the proposed Plot Plan **CONSISTENT** with the 2007 French Valley Airport Land Use Compatibility Plan, as amended in 2011, subject to the conditions included herein.

PROJECT DESCRIPTION: A proposal to construct four self-storage buildings totaling 129,816 square feet and a 1,135 square foot office building on 5.18. The applicant also proposes to remove previously proposed car wash and RV storage.

On March 10, 2016, the Commission found consistent ALUC Case ZAP1066FV16 a proposal to construct four self-storage buildings totaling 118,912 square feet, 3,129 square foot office building with mezzanines, and a 4,795 square foot carwash on 7.90 acres. The applicant also proposed changing the sites zoning from Industrial Park (IP) to Manufacturing-Service Commercial (MSC). The applicant also proposed to divide 12.76 acres into four parcels.

PROJECT LOCATION: The site is located easterly of Leon Road and westerly of Winchester Road, approximately 7,284 feet northeasterly of the northerly terminus of Runway 18-36 at French Valley Airport.

BACKGROUND:

Non-Residential Average-Acre Intensity:

Pursuant to the French Valley Airport Land Use Compatibility Plan, the project site is located within

Compatibility Zone C, where Additional Compatibility Policy 2.3 restricts non-residential average intensity to 80 people per acre.

Pursuant to Appendix C, Table C-1 of the Riverside County Airport Land Use Compatibility Plan, the following rates were used to calculate projected occupancy for the proposed building:

- Storage area – 1 person per 300 square feet, and
- Office area – 1 person per 200 square feet

The project proposes to construct four self-storage buildings totaling 129,816 square feet and a 1,135 square foot office building, consisting of 96,065 square feet of storage area and 1,135 square foot office building, accommodating 326 people, resulting in an average intensity of 63 people per acre, which is consistent with Zone C average intensity criterion of 80 people per acre.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle and 1.0 for commercial vehicles). Based on the number of parking spaces provided (6 standard vehicles) and (4 trailer spaces) the total occupancy would be estimated at 13 people for an average intensity of 3 people per acre, which is consistent with the Compatibility Zone C average intensity criterion of 80 people per acre.

Non-Residential Single-Acre Intensity: Pursuant to the French Valley Airport Land Use Compatibility Plan, the project site is located within Compatibility Zones C, where Additional Compatibility Policy 2.3 restricts single acre intensity to a maximum of 160 persons.

Based on the proposed site plan and occupancies as previously noted, the maximum single-acre intensity includes 4,505 square feet of storage area in Building A, 5,375 square feet of storage area in Building C, 11,275 square feet of first floor storage area in Building C, 11,225 square feet of second floor storage mezzanine area, 12,725 square feet of third floor storage mezzanine area, and 1,135 square feet of office area, accommodating a total occupancy of 156 people, which is consistent with the Zone C single-acre intensity criterion of 160 people.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zone C.

Noise: The French Valley Airport Land Use Compatibility Plan depicts the site as being located within below 55 - 60 CNEL contour range from aircraft noise. Therefore, no special mitigation of noise from aircraft is required.

Part 77: The elevation of Runway 18-36 at its northerly terminus is 1,347 feet above mean sea level. At a distance of approximately 7,284 feet from the runway to the site, Federal Aviation Administration (FAA) review would be required for any structures with a top of point elevation exceeding 1,420 feet AMSL. The site's finished floor elevation is 1,350 feet AMSL and the proposed structure height is 37 feet, for a top point elevation of 1,387 feet AMSL. Therefore, review by the FAA Obstruction Evaluation Service (FAA OES) was not required.

Open Area: Pursuant to the French Valley Airport Land Use Compatibility Plan, the project site is located within Compatibility Zone C, which requires projects greater than 10 acres to designate 20% of the project area as ALUC qualifying open area that could potentially serve as emergency landing

areas. The proposed project is 5.18 acres, therefore open area is not required.

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33C). The nearest portion of the project is located 7,284 feet from the runway, and therefore would be subject to the above requirement.

The project includes two bioretention basins totaling 3,840 square feet and pursuant to the study "Wildlife Hazard Management at Riverside County Airports: Background and Policy", October 2018, by Mead & Hunt, "Airports, Wildlife and Stormwater Management", such basins are permissible in Zone C when vegetation is selected careful so as not to provide food, shelter, nesting, roosting, or water for wildlife. The applicant also proposes two underground detention systems which will not contain surface water or attract wildlife and, therefore, would not constitute a hazard to flight. The project has been conditioned to be consistent with the basin criteria (as well as providing 48-hour draw down of the basin).

CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, outdoor production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators).
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.

- (e) Children's schools, day care centers, libraries, hospitals, nursing homes, places of worship, buildings with more than three aboveground habitable floors.
 - (f) Highly noise-sensitive outdoor nonresidential uses.
 - (g) Any use which results in a hazard to flight, including physical (e.g., tall objects), visual, and electronic forms of interference with the safety of aircraft operations.
3. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property and be recorded as a deed notice.
4. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the stormwater basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

5. This project has been evaluated as consisting of a proposal to construct four self-storage buildings totaling 129,816 square feet, and a 1,135 square foot office building on 5.18 acres. Any increase in building area, change in use to any higher intensity use, change in building location, or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS

PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



Legend

- Compatibility Zones**
- Airport Influence Area Boundary
 - Zone A
 - Zone B1
 - Zone B2
 - Zone C
 - Zone D
 - Zone E
- Boundary Lines**
- - - Airport Property Line
 - - - City Limits
 - ★ Height Review Overlay Zone

Note

Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

See Chapter 2, Table 2A from compatibility criteria associated with this map.

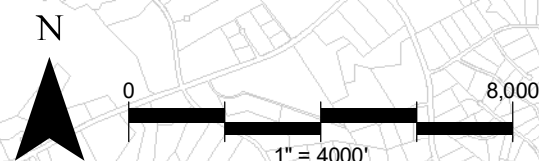
Riverside County
Airport Land Use Commission

Riverside County
Airport Land Use Compatibility Plan
Policy Document

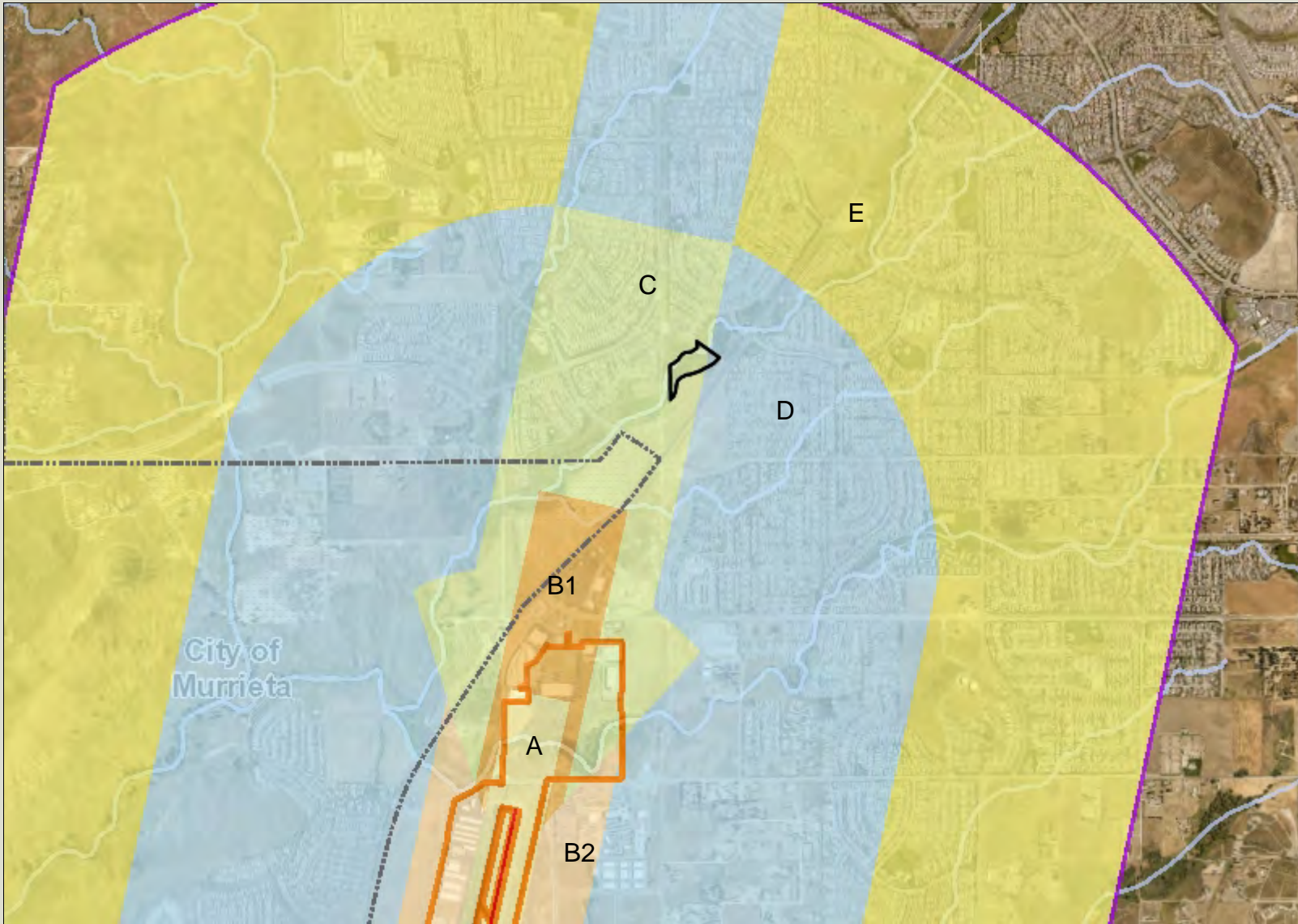
(April 2010)

Map FV-1

Compatibility Map
French Valley Airport



Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

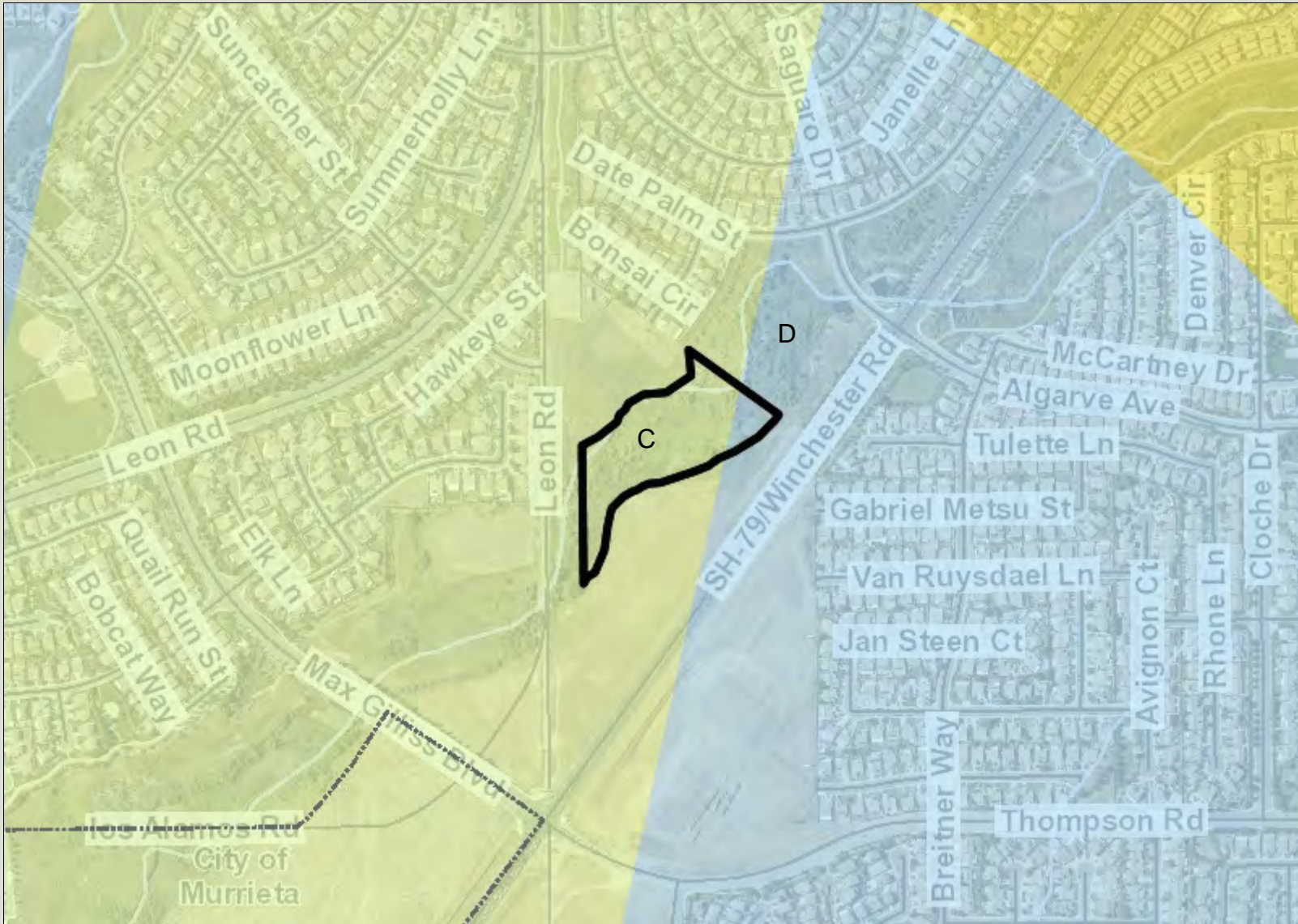


REPORT PRINTED ON... 4/26/2023 11:15:03 AM

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Notes

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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0 770 1,539 Feet

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Notes

Map My County Map



Legend

- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



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Notes

0 770 1,539 Feet

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Map My County Map



Legend

- County Centerline Names
- County Centerlines
- Blue Line Streams
- City Areas
- World Street Map



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Notes



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Map My County Map



- Legend**
- County Centerline Names
 - County Centerlines
 - Blueline Streams
 - City Areas
 - World Street Map

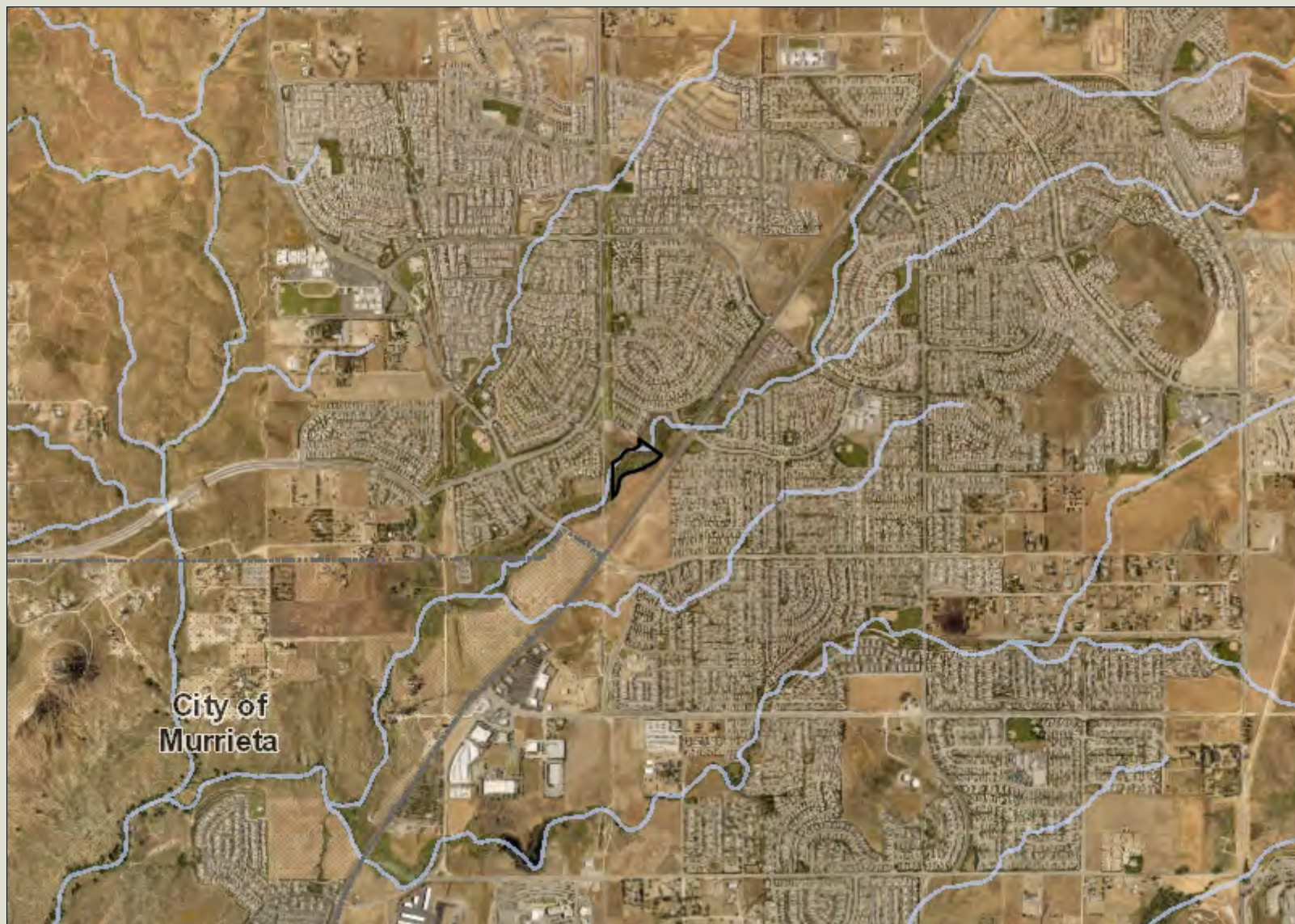


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Notes



Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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Notes



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VICINITY MAP

PUBLIC UTILITIES/ SERVICES

WATER: EASTERN MUNICIPAL WATER DISTRICT
 SEWER: EASTERN MUNICIPAL WATER DISTRICT
 TELEPHONE: VERIZON
 GAS: THE GAS COMPANY
 ELECTRIC: SOUTHERN CALIFORNIA EDISON CO.
 FIRE PROTECTION: RIVERSIDE COUNTY FIRE DEPARTMENT
 SCHOOL DISTRICT: MURRIETA VALLEY UNIFIED SCHOOL DIST.

SITE DATA

ASSESSOR'S PARCEL NUMBER: 480-179-001
 SITE AREA: 344,099 SQ. FT. (7.90) ACRES
 (PARCEL 1, 2, 4, & LOT A)
 PARCEL 1: 5.18 AC
 PARCEL 2: 1.96 AC
 PARCEL 4: 0.08 AC
 LOT A: 0.08 AC

DEVELOPER/OWNER

WEST COAST SELF-STORAGE
 30 CASTLETOWN ROAD
 PLEASANTON, CA 94566
 CONTACT: STEVE TANGNEY
 PHONE: 408-838-4543
 E-MAIL: stangney@wccselfstorage.com

MWB FRENCH VALLEY LLC
 10230 NE POINTS DRIVE
 SUITE 410
 KIRKLAND, WA 98033

ARCHITECT

WALLACE DESIGN GROUP
 27405 PUERTA REAL, SUITE 295
 MISSION VIEJO, CA 92691
 CONTACT: RICK WALLACE
 PHONE: 949-525-0229
 E-MAIL: rick@wallacedesigngroup.net

CIVIL ENGINEER

PROACTIVE ENGINEERING
 CONSULTANTS, INC.
 200 S. MAIN STREET, SUITE 900
 CORONA, CA 92682
 CONTACT: KIRK HEIN
 PHONE: 951-280-3300
 E-MAIL: khein@proeng.net

LANDSCAPE ARCH

BMLA LANDSCAPE ARCHITECTS
 310 NORTH JOY STREET
 CORONA, CA 92679
 CONTACT: JEFF TROJAN
 PHONE: 951-737-1124
 E-MAIL: jrt@bmla.net

BUILDING COVERAGE

95,487 SQ. FT. (19.1%)

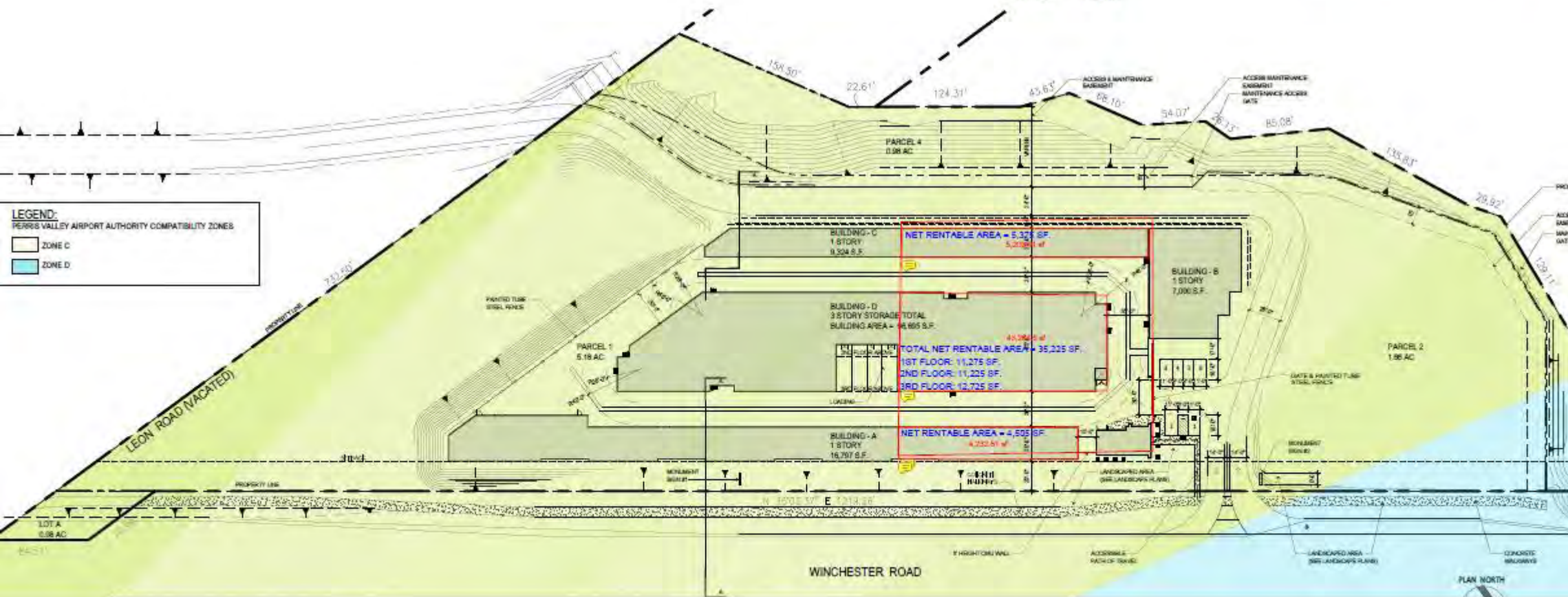
BUILDING DATA

BLDG. 'A' (1 STORY) 18,797 SQ. FT.
 BLDG. 'B' (1 STORY) 7,000 SQ. FT.
 BLDG. 'C' (1 STORY) 9,324 SQ. FT.
 BLDG. 'D' (3 STORY) 46,665 SQ. FT.
 STORAGE SUBTOTAL 130,815 SQ. FT.

OFFICE (1 STORY) 1,136 SQ. FT.
 SELF STORAGE TOTAL 130,951 SQ. FT.

HOURS OF OPERATION

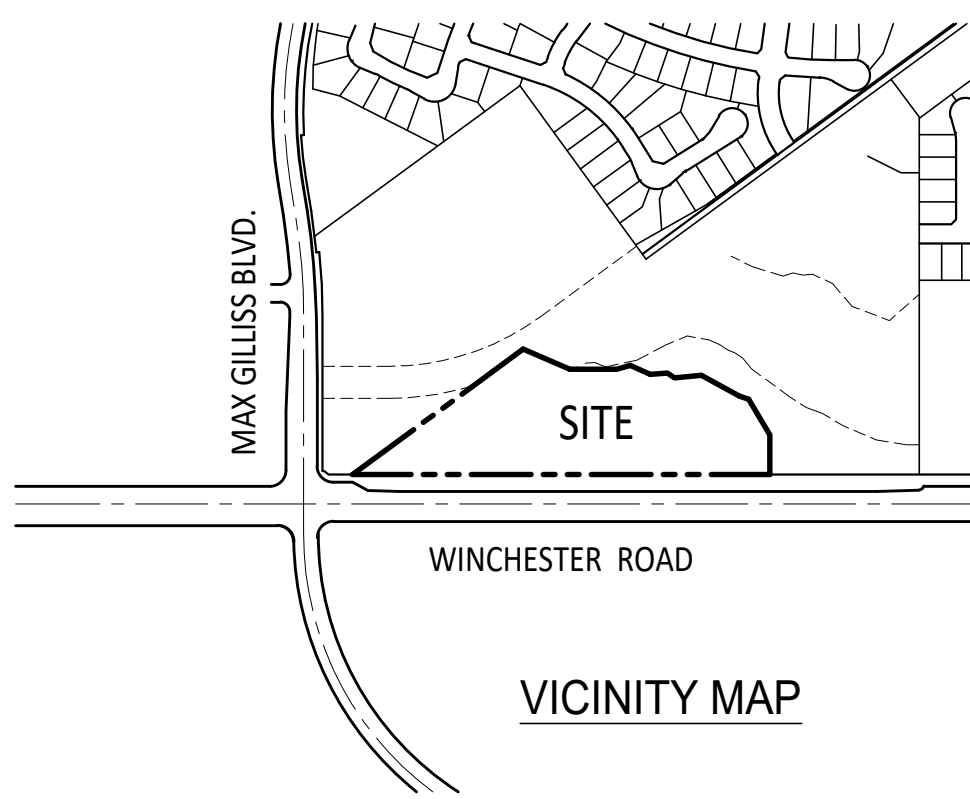
SELF STORAGE:
 MON. - FRI.: 7am to 7pm
 SAT. - SUN.: 9am to 5pm



LEGEND:
 PERRIS VALLEY AIRPORT AUTHORITY COMPATIBILITY ZONES

- ZONE C
- ZONE D





SHEET INDEX

1. OVERALL SITE PLAN
 2. ENLARGED ENTRY AREA SITE PLAN
 3. UNIT MIX PLANS- BUILDINGS A,B,C, & OFFICE FLOOR PLAN
 4. UNIT MIX PLAN- BUILDING D
 5. ROOF PLAN
 6. ELEVATIONS- BUILDING A
 7. ELEVATIONS- BUILDING A
 8. ELEVATIONS- BUILDING B & C
 9. ELEVATIONS- BUILDING D
 10. ELEVATIONS- BUILDING D
-
- L1 CONCEPTUAL LANDSCAPE PLAN
 - L2 CONCEPTUAL LANDSCAPE PLAN
 - L3 HYDROZONE MAP AND PLANT PALETTE

PUBLIC UTILITIES/ SERVICES

WATER: EASTERN MUNICIPAL WATER DISTRICT
 SEWER: EASTERN MUNICIPAL WATER DISTRICT
 TELEPHONE: VERIZON
 GAS: THE GAS COMPANY
 ELECTRIC: SOUTHERN CALIFORNIA EDISON CO.
 FIRE PROTECTION: RIVERSIDE COUNTY FIRE DEPARTMENT
 SCHOOL DISTRICT: MURRIETA VALLEY UNIFIED SCHOOL DIST.

SITE DATA

ASSESSOR'S PARCEL NUMBER: 480-170-001
 344,056 SQ. FT. (7.90) ACRES
 (PARCEL 1, 2, 4, & LOT A)
 PARCEL 1- 5.18 AC
 PARCEL 2- 1.66 AC
 PARCEL 4- 0.98 SC
 LOT A- 0.08 AC

DEVELOPER/OWNER

WEST COAST SELF-STORAGE
 39 CASTLETOWN ROAD
 PLEASANTON, CA 94566
 CONTACT: STEVE TANGNEY
 PHONE: 408-836-4543
 E-MAIL: stangney@wcselfstorage.com

NWB FRENCH VALLEY LLC
 10230 NE POINTS DRIVE
 SUITE 410
 KIRKLAND, WA 98033

ARCHITECT

WALLACE DESIGN GROUP
 27405 PUERTA REAL, SUITE 235
 MISSION VIEJO, CA 92691
 CONTACT: RICK WALLACE
 PHONE: 949-525-9229
 E-MAIL: rick@wallacedesigngroup.net

CIVIL ENGINEER

PROACTIVE ENGINEERING
 CONSULTANTS, INC.
 200 S. MAIN STREET, SUITE 300
 CORONA, CA 92882
 CONTACT: KIRK HEIN
 PHONE: 951-280-3300
 E-MAIL: khein@proeng.net

LANDSCAPE ARCHITECT

BMLA LANDSCAPE ARCHITECTURE
 310 NORTH JOY STREET
 CORONA, CA 92879
 CONTACT: JEFF TROJANOWSKI
 PHONE: 951-737-1124 x 128
 E-MAIL: jeff@bmla.net

BUILDING COVERAGE 65,487 SQ. FT. (19.1%)

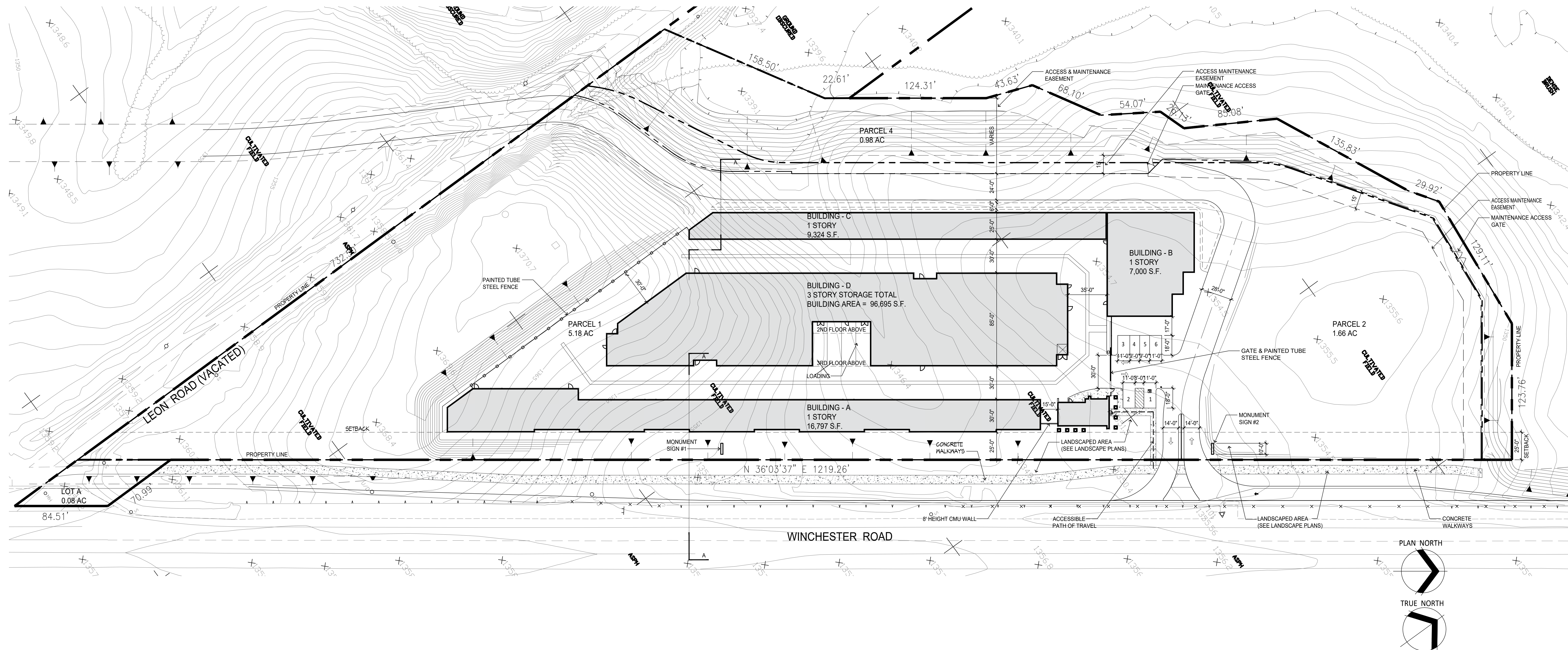
BUILDING DATA

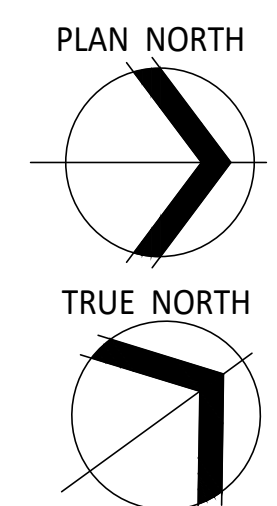
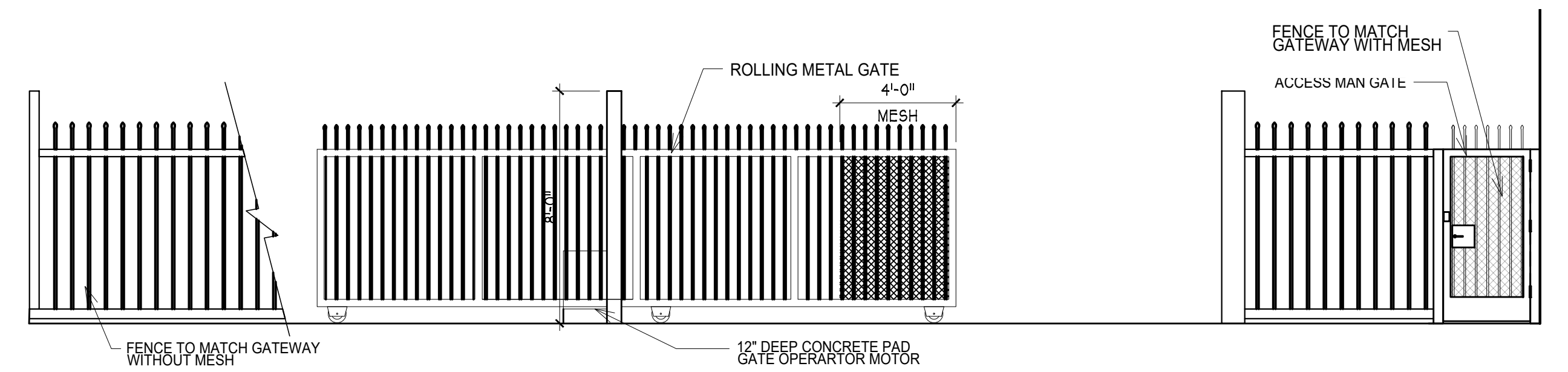
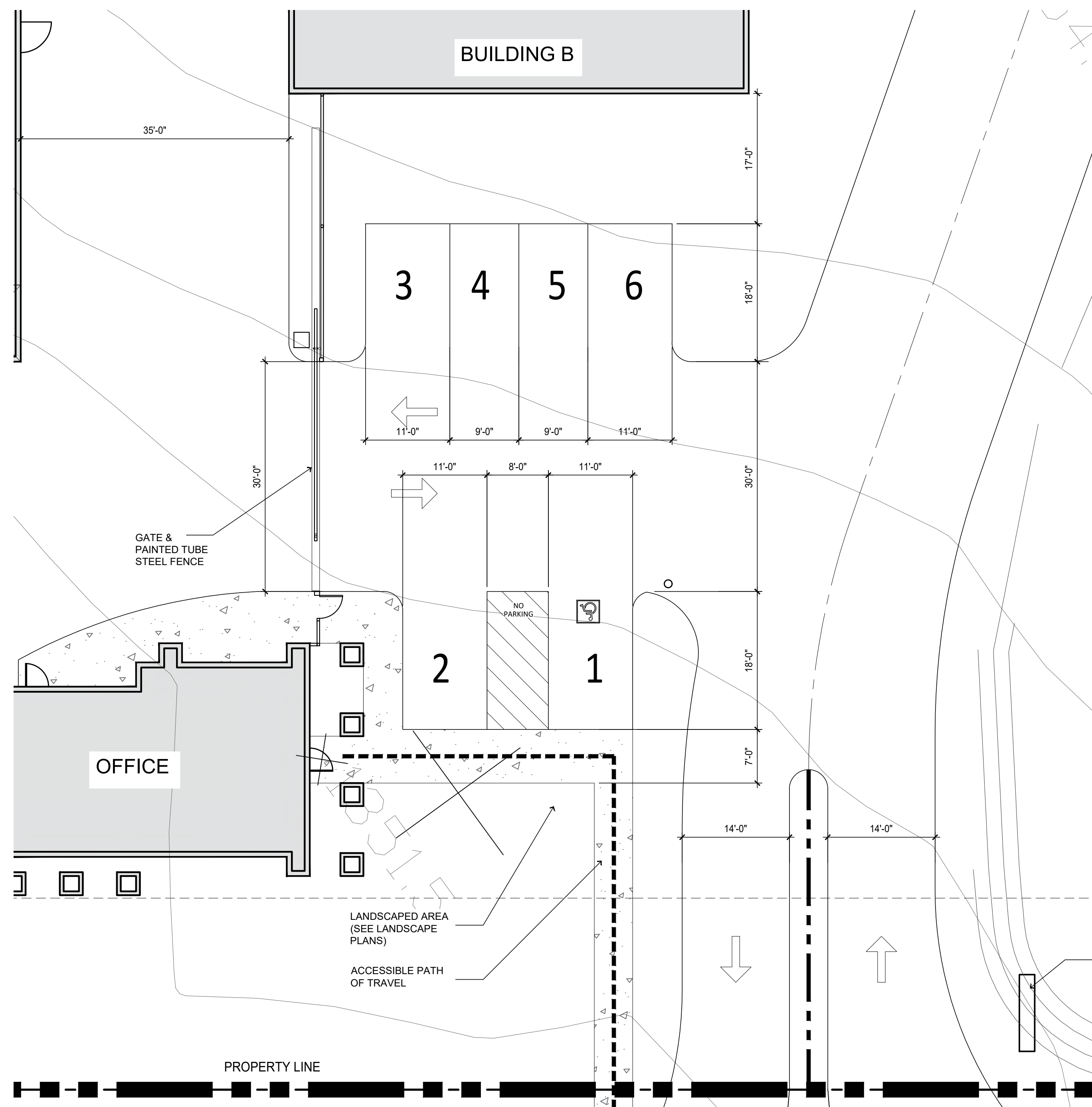
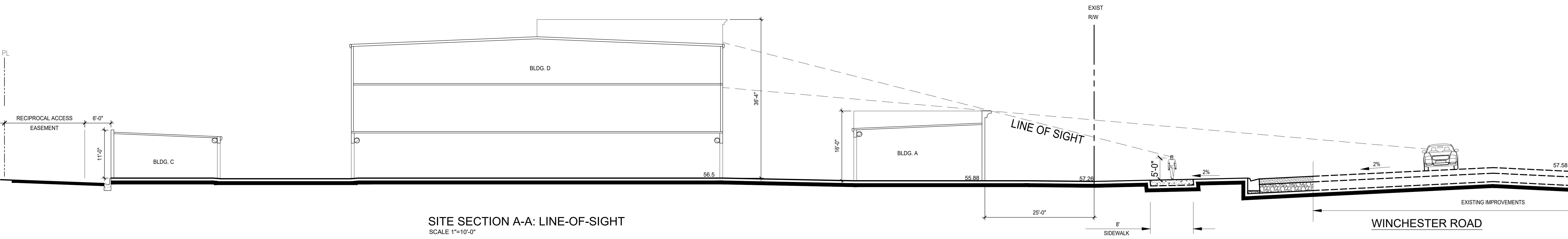
BLDG. 'A'	(1 STORY)	16,797 SQ. FT.
BLDG. 'B'	(1 STORY)	7,000 SQ. FT.
BLDG. 'C'	(1 STORY)	9,324 SQ. FT.
BLDG. 'D'	(3 STORY)	96,695 SQ. FT.
STORAGE SUBTOTAL		129,816 SQ. FT.

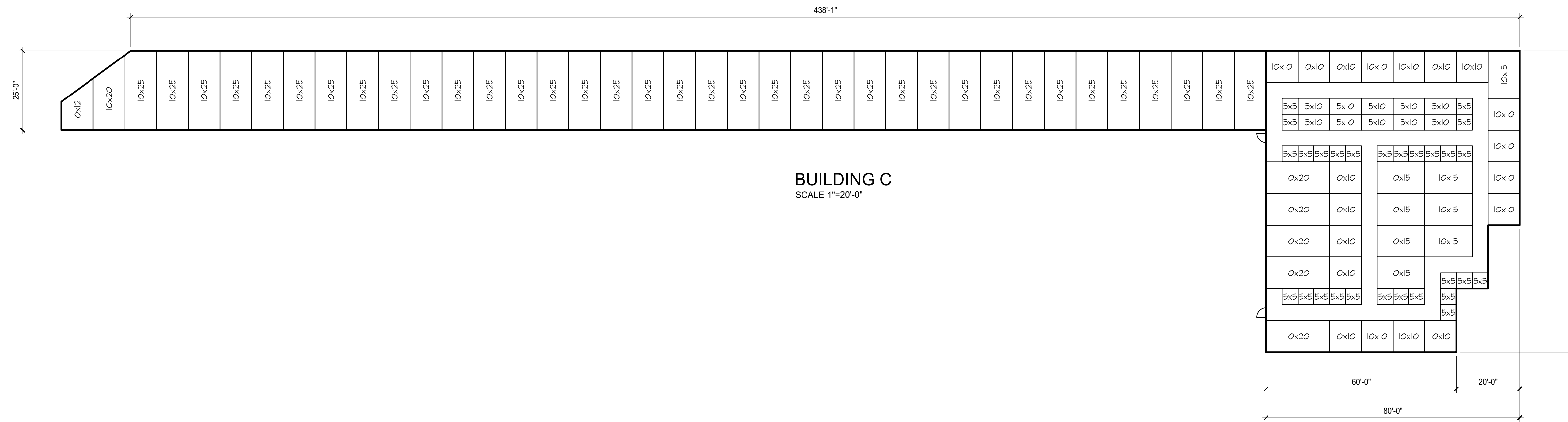
OFFICE	(1 STORY)	1,135 SQ. FT.
SELF STORAGE TOTAL		130,951 SQ. FT.

HOURS OF OPERATION

SELF STORAGE:
 MON. - FRI.: 7am to 7pm
 SAT. - SUN.: 9am to 5pm

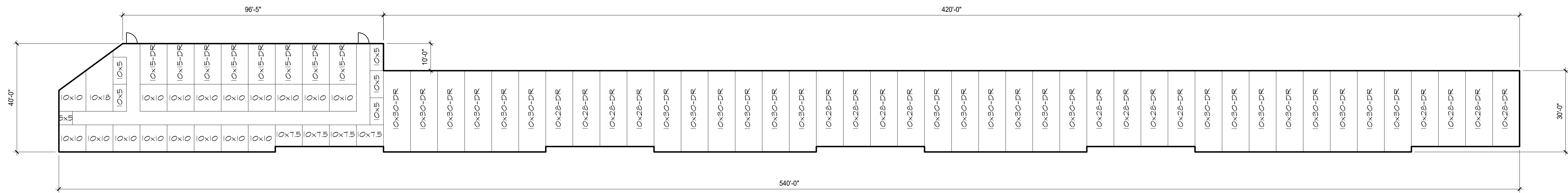




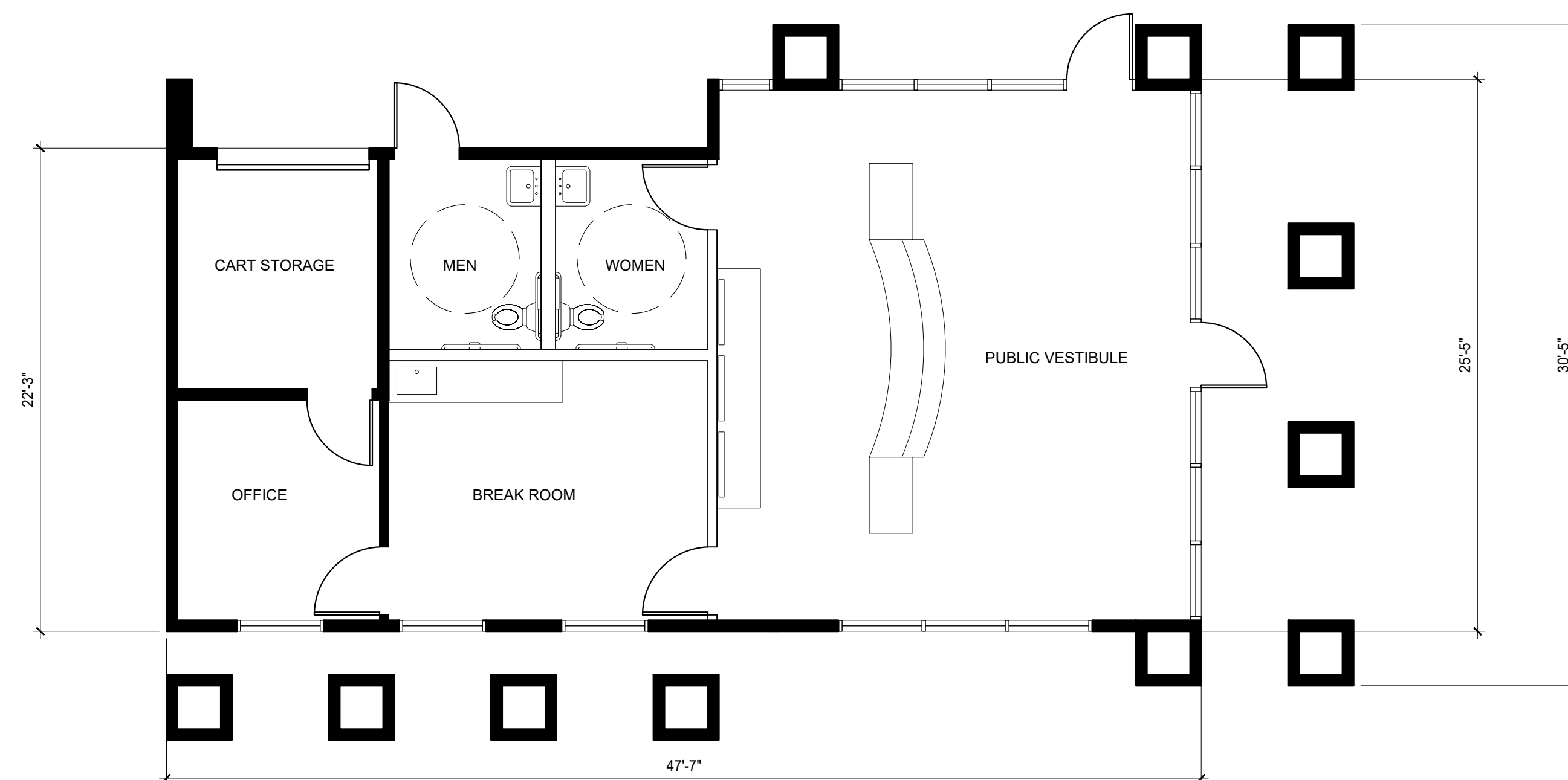


BUILDING C
SCALE 1"=20'-0"

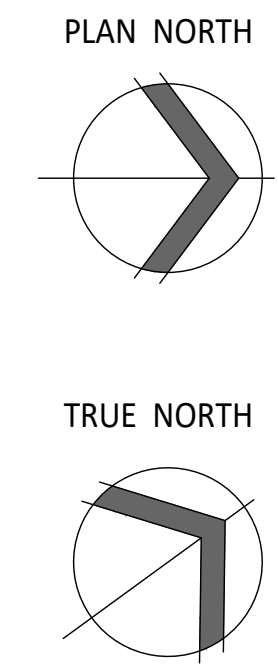
BUILDING B
SCALE 1"=20'-0"

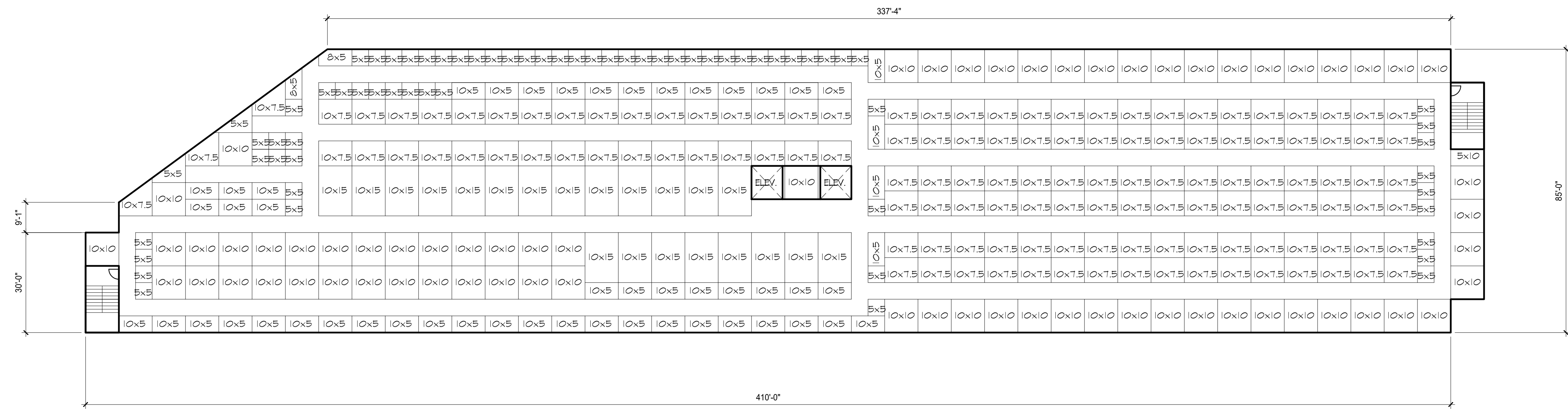


BUILDING A
SCALE 1"=20'-0"

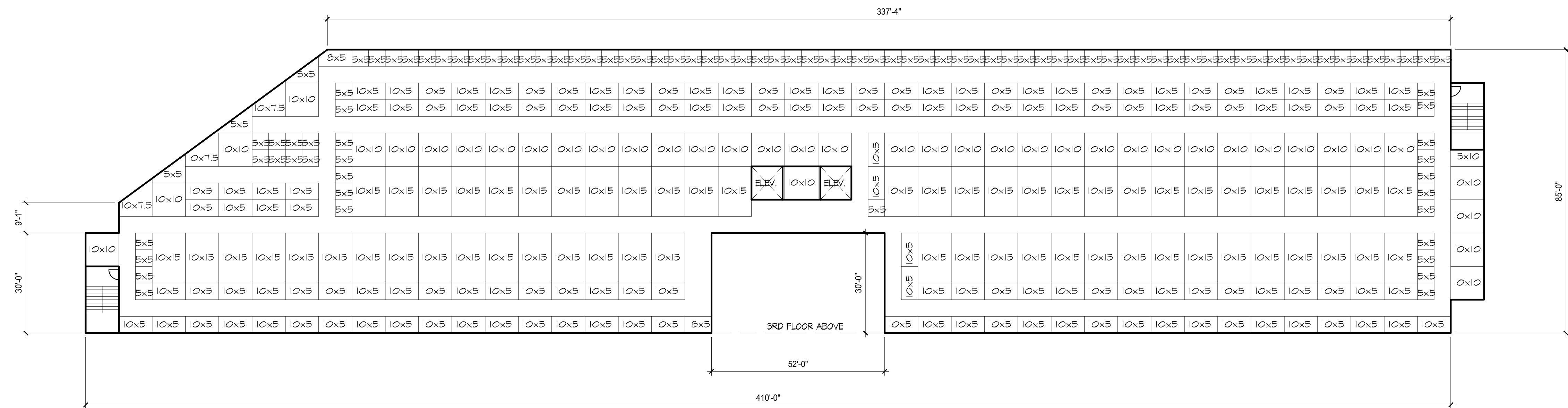


OFFICE- FLOOR PLAN
SCALE 3/16"=1'-0"

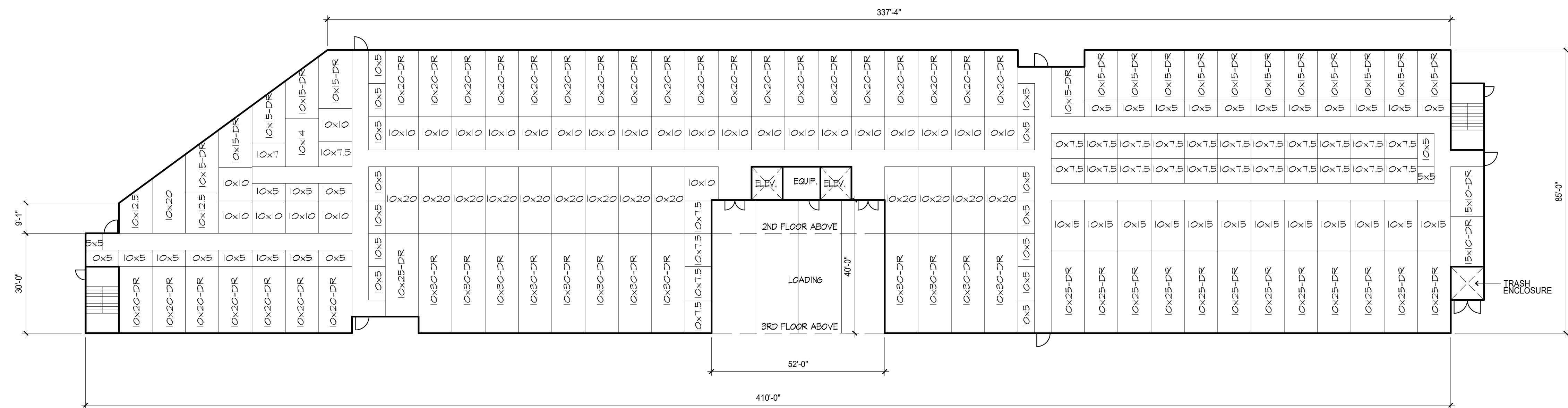




BUILDING D- THIRD FLOOR PLAN
SCALE 1"=20'-0"

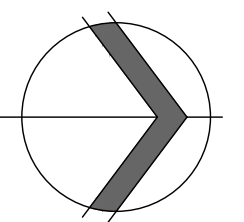


BUILDING D- SECOND FLOOR PLAN
SCALE 1"=20'-0"

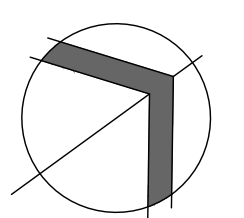


BUILDING D- FIRST FLOOR PLAN
SCALE 1"=20'-0"



PLAN NORTH

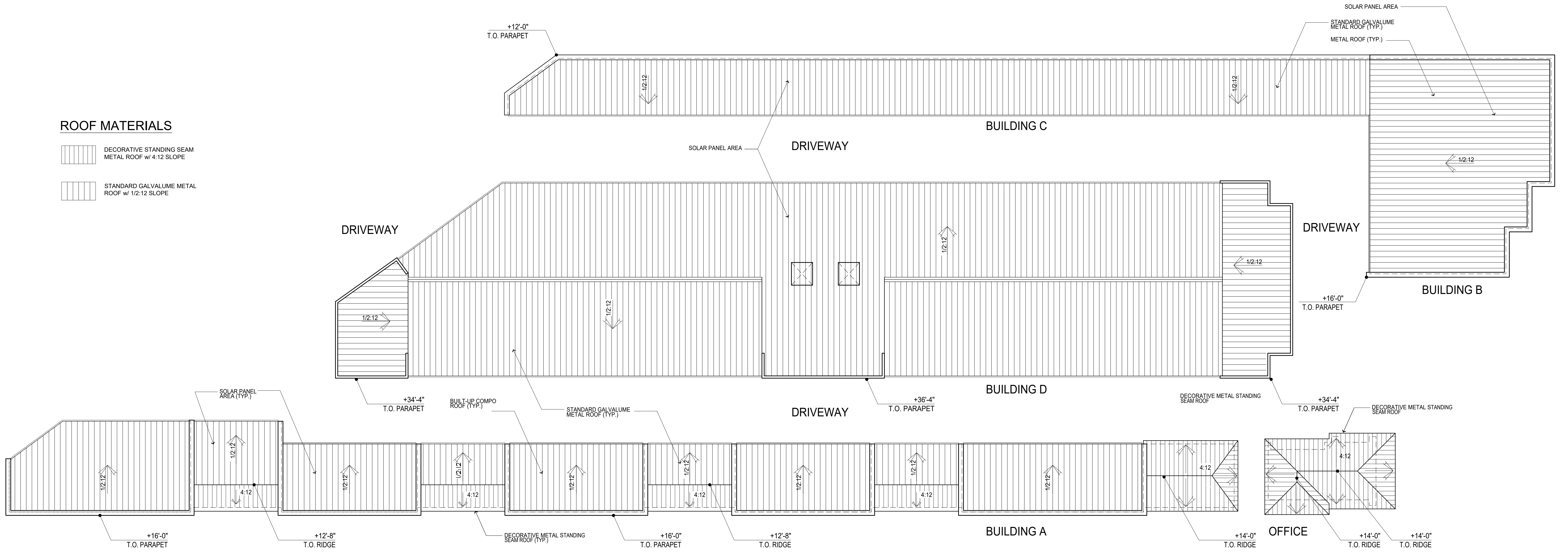


TRUE NORTH

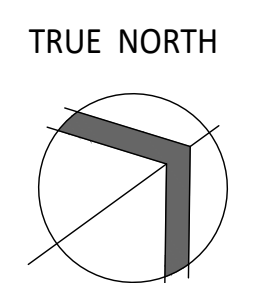
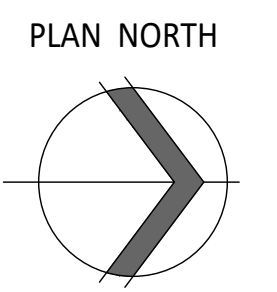


ROOF MATERIALS

-  DECORATIVE STANDING SEAM METAL ROOF w/ 4:12 SLOPE
-  STANDARD GALVALUME METAL ROOF w/ 1/2:12 SLOPE

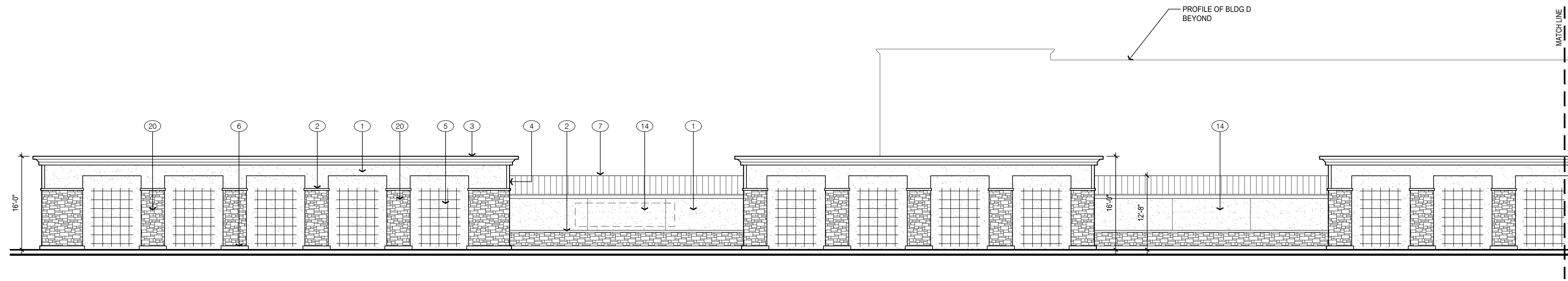


ROOF PLAN
SCALE 1"=20'-0"

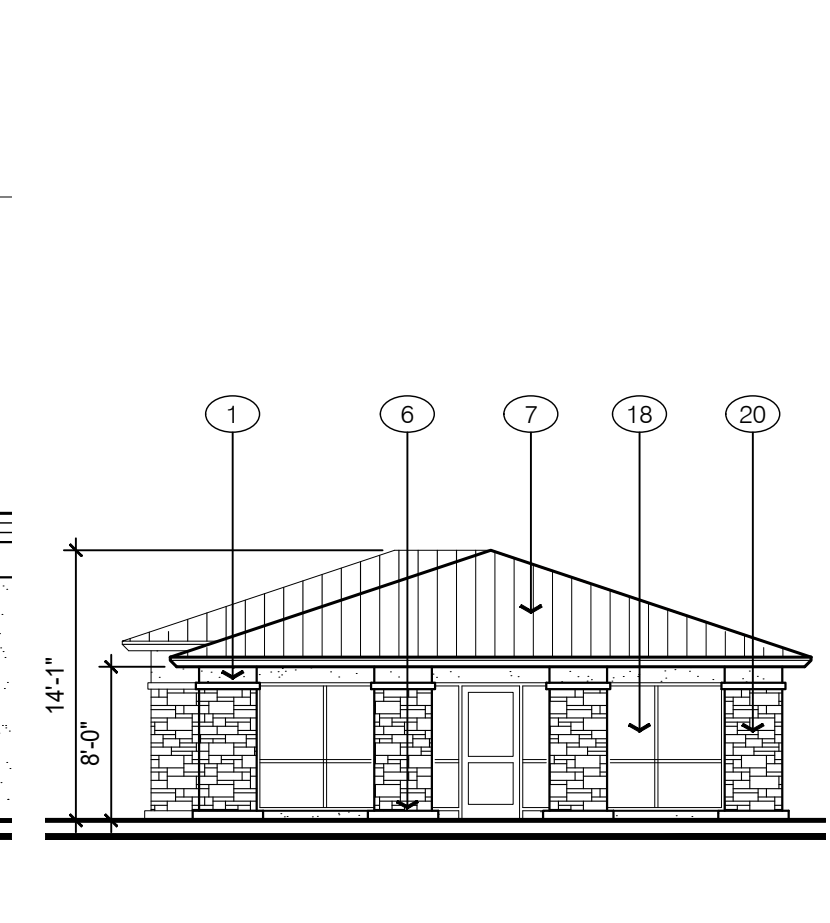


ELEVATION KEYNOTES

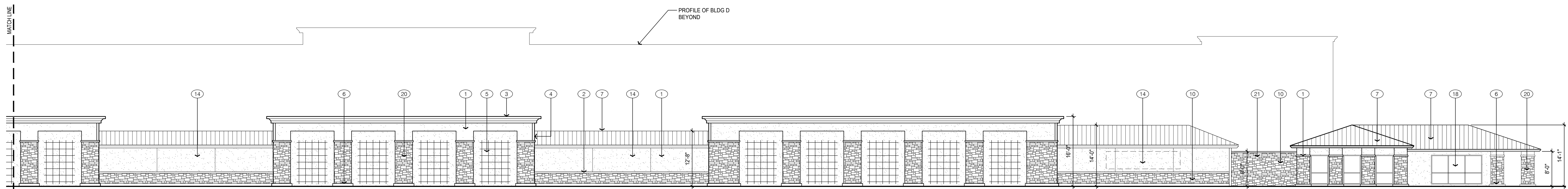
1. STUCCO FINISH
2. STUCCO SCORE JOINT
3. FOAM CORNICE WITH STUCCO FINISH
4. FOAM CORNER TRIM WITH STUCCO FINISH
5. METAL VINE TRELLIS IN RECESSED PLANE
6. LEDGERSTONE VENEER BASE
7. STANDING SEAM METAL ROOF
8. HORIZONTAL METAL EXTERIOR SIDING TO MATCH MBCI PBR PANEL
9. EXTERIOR GLASS STOREFRONT SYSTEM
10. STONE VENEER O/ CMU
11. CMU DOOR PILASTER WITH ELASTOMERIC PAINT FINISH
12. METAL ROLL UP DOOR
13. METAL GUTTER
14. PROPOSED SIGN LOCATION
15. HALLWAY EXIT DOOR
16. STAIRWELL EXIT DOOR
17. WINDOW
18. STOREFRONT GLASS AT OFFICE
19. PARAPET BEYOND
20. LEDGERSTONE VENEER



BUILDING A - EAST ELEVATION (SOUTH END)
SCALE 1"=10'-0"

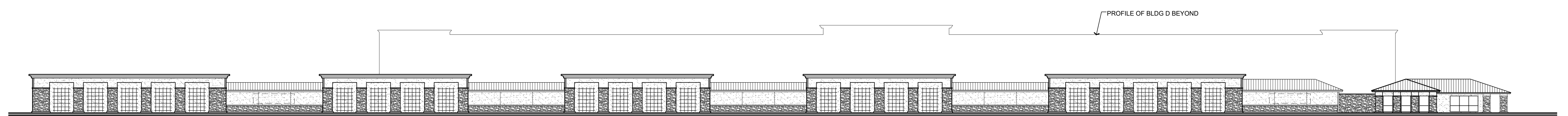


OFFICE - NORTH ELEVATION
SCALE 1"=10'-0"



BUILDING A - EAST ELEVATION (NORTH END)
SCALE 1"=10'-0"

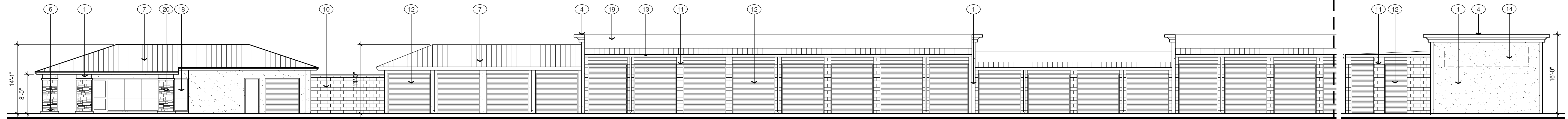
OFFICE - EAST ELEVATION
SCALE 1"=10'-0"



BUILDING A & OFFICE - OVERALL EAST ELEVATION
SCALE 1"=20'-0"

ELEVATION KEYNOTES

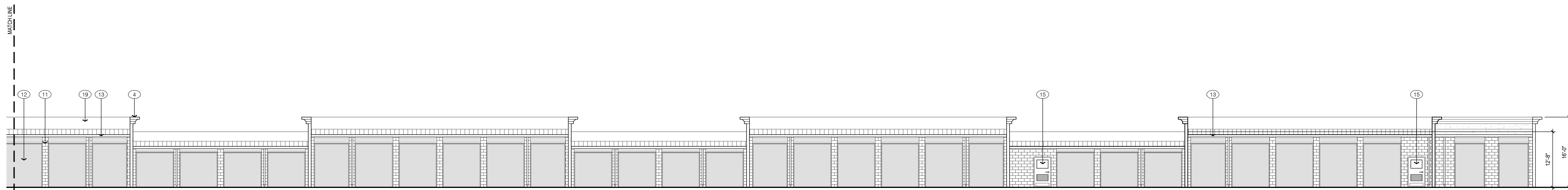
1. STUCCO FINISH
2. STUCCO SCORE JOINT
3. FOAM CORNICE WITH STUCCO FINISH
4. FOAM CORNER TRIM WITH STUCCO FINISH
5. METAL VINE TRELLIS IN RECESSED PLANE
6. LEDGERSTONE VENEER BASE
7. STANDING SEAM METAL ROOF
8. HORIZONTAL METAL EXTERIOR SIDING TO MATCH MBCI PBR PANEL
9. EXTERIOR GLASS STOREFRONT SYSTEM
10. STONE VENEER O/ CMU
11. CMU DOOR PILASTER WITH ELASTOMERIC PAINT FINISH
12. METAL ROLL UP DOOR
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16. STAIRWELL EXIT DOOR
17. WINDOW
18. STOREFRONT GLASS AT OFFICE
19. PARAPET BEYOND
20. LEDGERSTONE VENEER



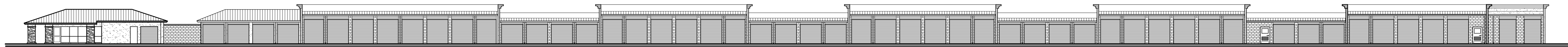
OFFICE - WEST ELEVATION
SCALE 1"=10'-0"

BUILDING A - WEST ELEVATION (NORTH END)
SCALE 1"=10'-0"

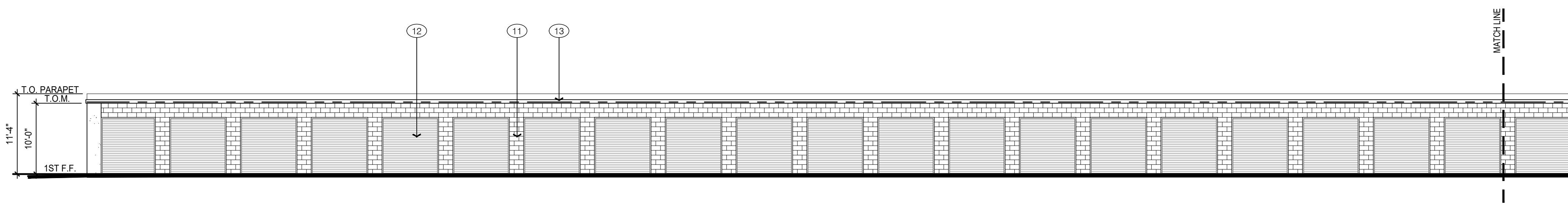
BUILDING A - SOUTH ELEVATION
SCALE 1"=10'-0"



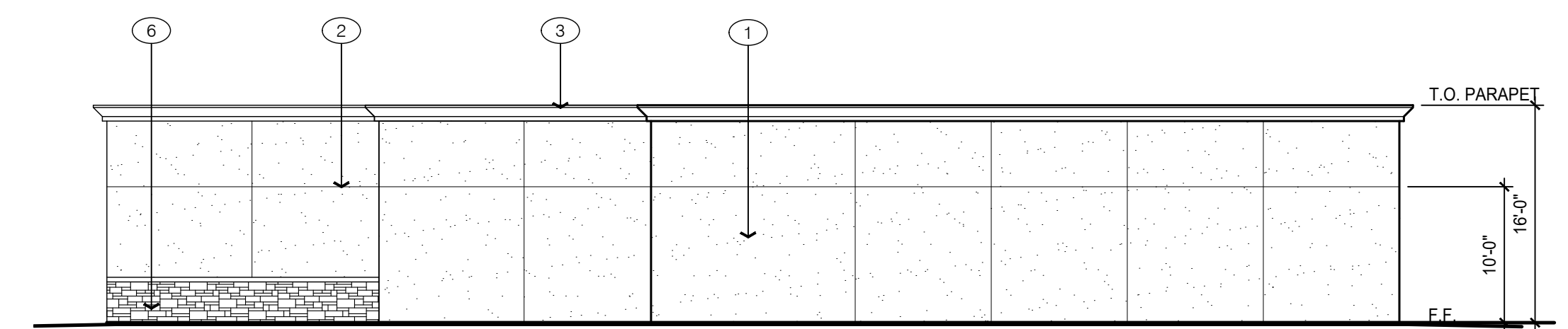
BUILDING A - WEST ELEVATION (SOUTH END)
SCALE 1"=10'-0"



BUILDING A - OVERALL WEST ELEVATION
SCALE 1"=20'-0"

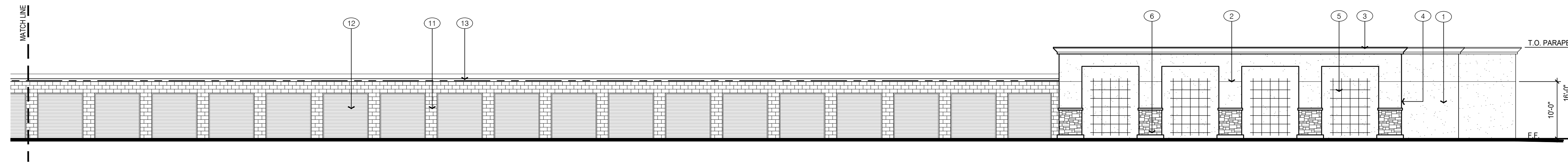


BUILDING B AND C - EAST ELEVATION (SOUTH END)
SCALE 1"=10'-0"

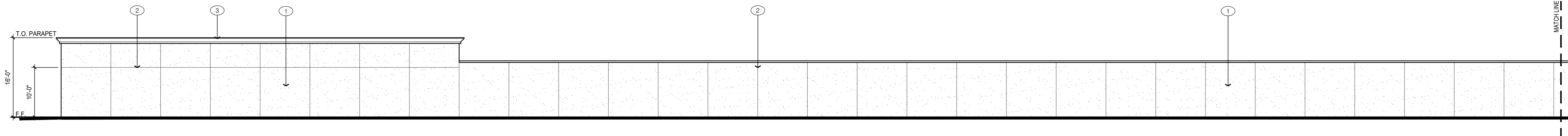


BUILDING B - NORTH ELEVATION
SCALE 1"=10'-0"

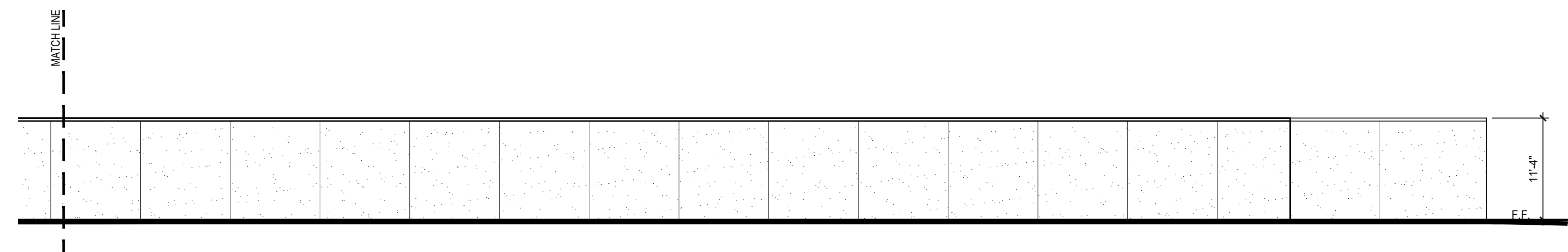
- ELEVATION KEYNOTES**
1. STUCCO FINISH
 2. STUCCO SCORE JOINT
 3. FOAM CORNICE WITH STUCCO FINISH
 4. FOAM CORNER TRIM WITH STUCCO FINISH
 5. METAL VINE TRELLIS IN RECESSED PLANE
 6. LEDGERSTONE VENEER BASE
 7. STANDING SEAM METAL ROOF
 8. HORIZONTAL METAL EXTERIOR SIDING TO MATCH MBCI PBR PANEL
 9. EXTERIOR GLASS STOREFRONT SYSTEM
 10. STONE VENEER O/ CMU
 11. CMU DOOR PILASTER WITH ELASTOMERIC PAINT FINISH
 12. METAL ROLL UP DOOR
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 14. PROPOSED SIGN LOCATION
 15. HALLWAY EXIT DOOR
 16. STAIRWELL EXIT DOOR
 17. WINDOW
 18. STOREFRONT GLASS AT OFFICE
 19. PARAPET BEYOND
 20. LEDGERSTONE VENEER



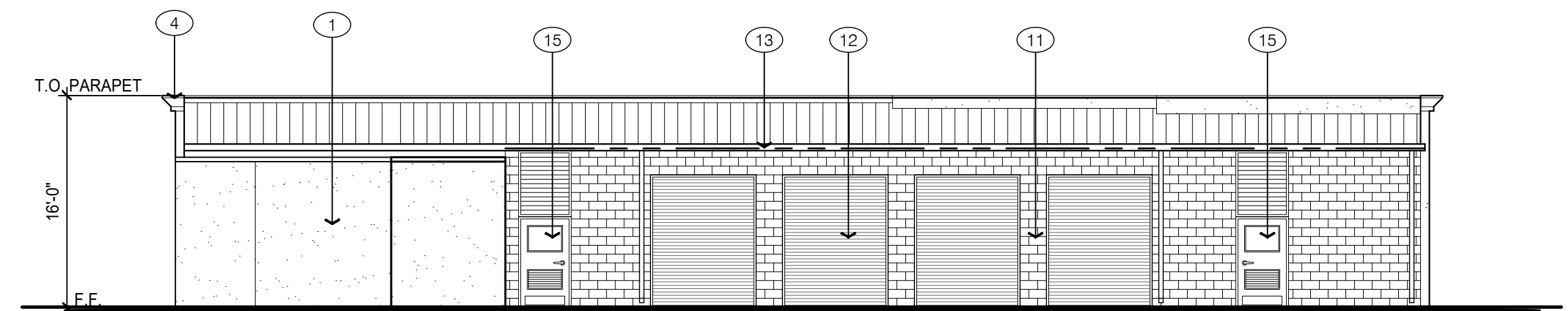
BUILDING B AND C - EAST ELEVATION (NORTH END)
SCALE 1"=10'-0"



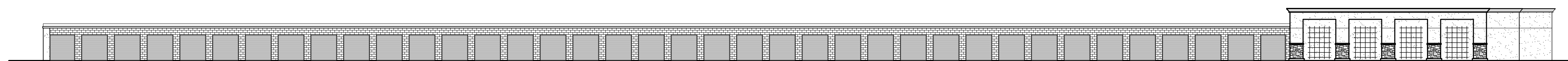
BUILDING B AND C - EAST ELEVATION (NORTH END)
SCALE 1"=10'-0"



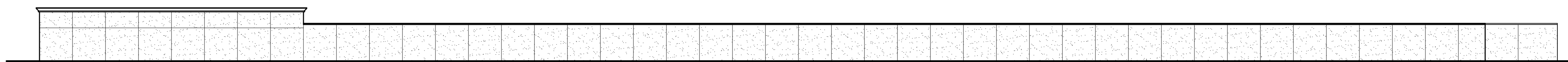
BUILDING B AND C - EAST ELEVATION (SOUTH END)
SCALE 1"=10'-0"



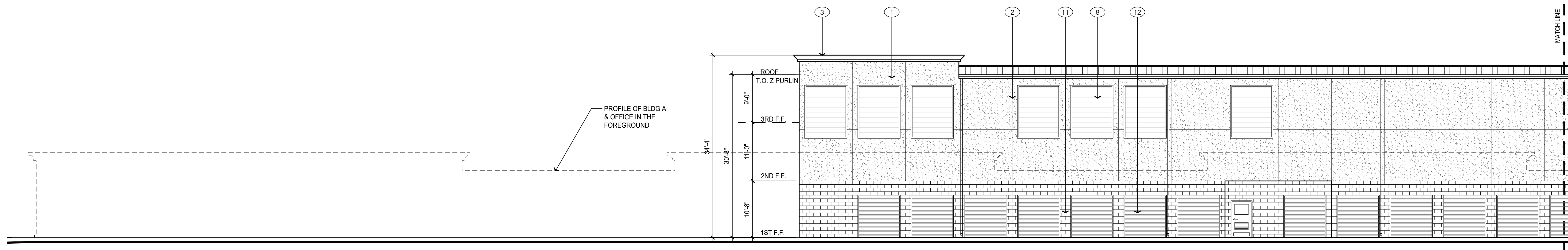
BUILDING B AND C - SOUTH ELEVATION
SCALE 1"=10'-0"



BUILDING B AND C - OVERALL EAST ELEVATION
SCALE 1"=20'-0"



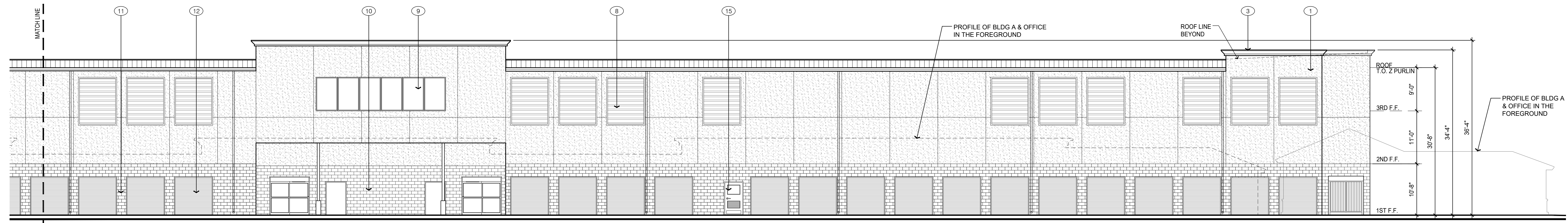
BUILDING B AND C - OVERALL WEST ELEVATION
SCALE 1"=20'-0"



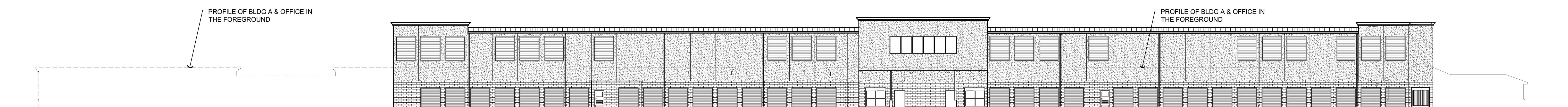
BUILDING D - EAST ELEVATION (SOUTH END)
SCALE 1"=10'-0"

ELEVATION KEYNOTES

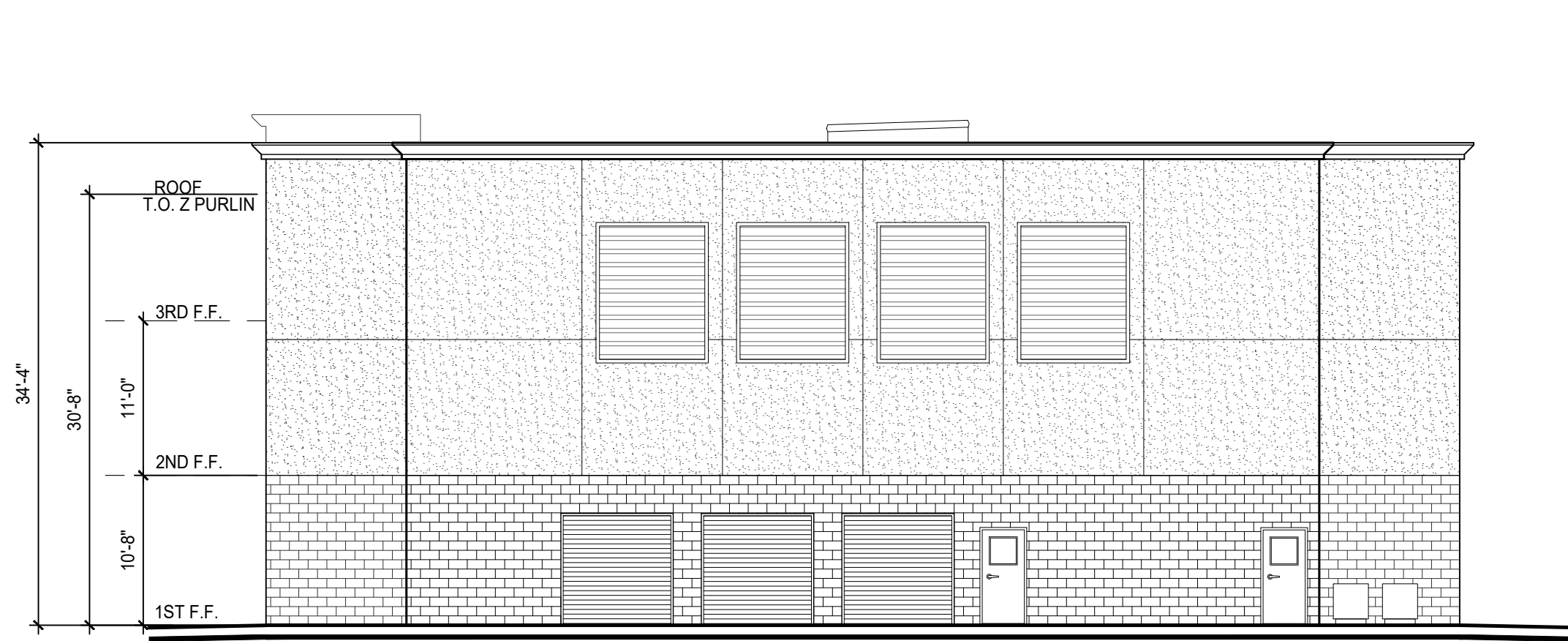
1. STUCCO FINISH
2. STUCCO SCORE JOINT
3. FOAM CORNICE WITH STUCCO FINISH
4. FOAM CORNER TRIM WITH STUCCO FINISH
5. METAL VINE TRELLIS IN RECESSED PLANE
6. LEDGERSTONE VENEER BASE
7. STANDING SEAM METAL ROOF
8. HORIZONTAL METAL EXTERIOR SIDING TO MATCH MBCI PBR PANEL
9. EXTERIOR GLASS STOREFRONT SYSTEM
10. STONE VENEER O/ CMU
11. CMU DOOR PILASTER WITH ELASTOMERIC PAINT FINISH
12. METAL ROLL UP DOOR
13. METAL GUTTER
14. PROPOSED SIGN LOCATION
15. HALLWAY EXIT DOOR
16. STAIRWELL EXIT DOOR
17. WINDOW
18. STOREFRONT GLASS AT OFFICE
19. PARAPET BEYOND
20. LEDGERSTONE VENEER



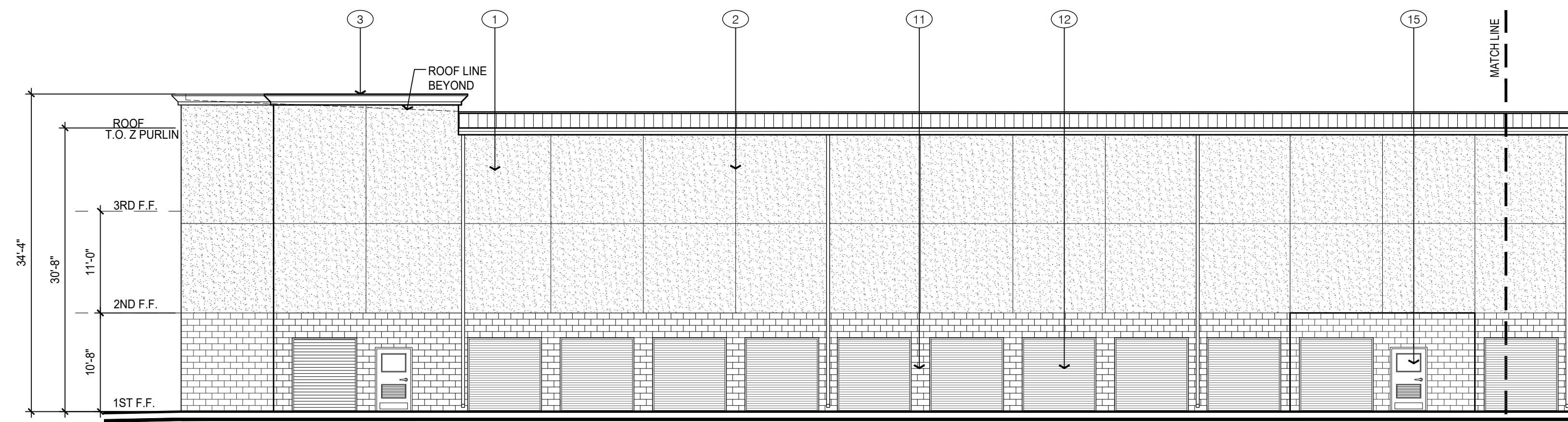
BUILDING D - EAST ELEVATION (NORTH END)
SCALE 1"=10'-0"



BUILDING D - OVERALL EAST ELEVATION
SCALE 1"=20'-0"

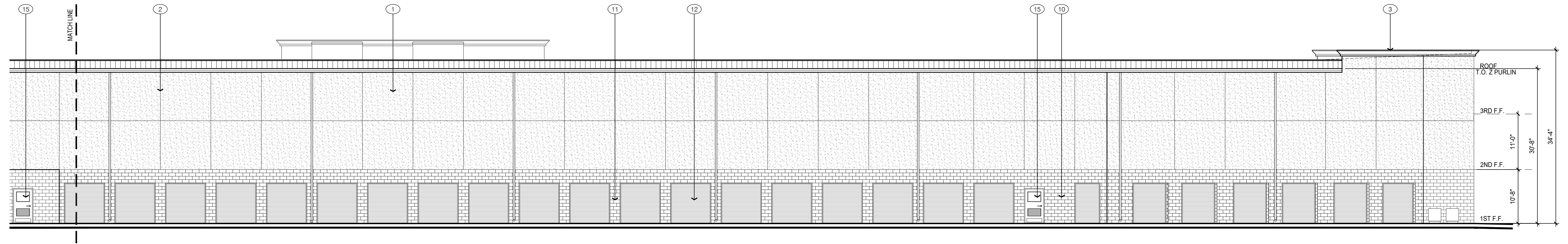


BUILDING D - NORTH ELEVATION
SCALE 1"=10'-0"

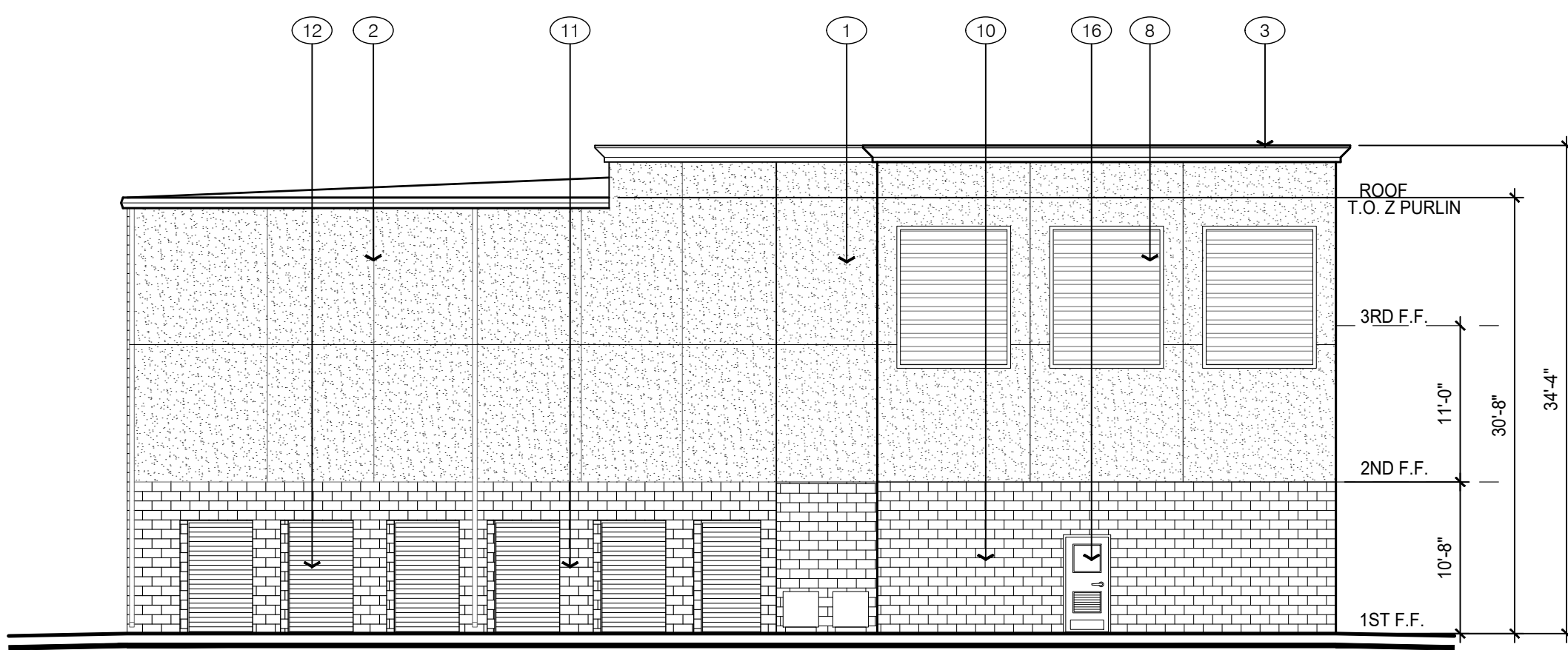


BUILDING D - WEST ELEVATION (NORTH END)
SCALE 1"=10'-0"

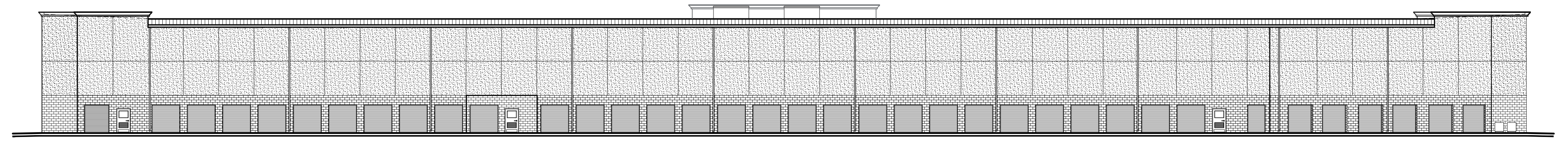
- ELEVATION KEYNOTES**
1. STUCCO FINISH
 2. STUCCO SCORE JOINT
 3. FOAM CORNICE WITH STUCCO FINISH
 4. FOAM CORNER TRIM WITH STUCCO FINISH
 5. METAL VINE TRELLIS IN RECESSED PLANE
 6. LEDGERSTONE VENEER BASE
 7. STANDING SEAM METAL ROOF
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 16. STAIRWELL EXIT DOOR
 17. WINDOW
 18. STOREFRONT GLASS AT OFFICE
 19. PARAPET BEYOND
 20. LEDGERSTONE VENEER



BUILDING D - WEST ELEVATION (SOUTH END)
SCALE 1"=10'-0"



BUILDING D - SOUTH ELEVATION
SCALE 1"=10'-0"



BUILDING D - OVERALL WEST ELEVATION
SCALE 1"=20'-0"

NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact **ALUC Planner Jackie Vega at (951) 955-0982**. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The Riverside County Planning Department should be contacted on non-ALUC issues. For more information please contact Riverside County Planner Jennifer Lopez at (951) 955-3107.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to javega@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please telephone Barbara Santos at (951) 955-5132.

PLACE OF HEARING: **Riverside County Administration Center**
 4080 Lemon Street, 1st Floor Board Chambers
 Riverside California

DATE OF HEARING: **June 8, 2023**

TIME OF HEARING: **9:30 A.M.**

CASE DESCRIPTION:

ZAP1125FV23 – Wallace Design Group (Representative: West Coast Self Storage) – County of Riverside Case No. PP20682R01 (Plot Plan Revision). A proposal to construct four self-storage buildings totaling 129,816 square feet and a 1,135 square foot office building on 5.18, located easterly of Leon Road and westerly of Winchester Road. The applicant also proposes to remove previously proposed car wash and RV storage. (ZAP1066FV16 was previously found consistent by the Commission on this site) (Airport Compatibility Zones C of the French Valley Airport Influence Area).



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC STAFF ONLY

ALUC Case Number: ZAP1125FV23

Date Submitted: 4/20/23

AIA:

French Valley



Zone: Zone C&D

Public Hearing



Staff Review



Applicant

Applicant

Full Name: ENRIQUE WALLACE III

Applicant Address: 27405 PUERTA REAL, SUITE 235 MISSION VIEJO, CA 92691

Phone: 949-525-9229

Email: rick@wallacedesigngroup.net

Representative/ Property Owner Contact Information

Representative: Steve Tangney

Email: stangney@wcselfstorage.com

VP, West Coast Self Storage

Phone: 408-836-4543

Address: 39 Castledown Rd Pleasanton Ca 94566

Property
Owner:

NWB French Valley, A Washington Limited Liability Co

Email: rwright@nwbl.com

Attn: Roger Wright

Phone: (206) 652-8781

Address: 10230 NE Points Drive Suite 410 Kirkland, WA 98033

Local Jurisdiction Agency

Agency
Name:

Riverside County Planning

Phone: 951-955-3107

Staff Contact: Jennifer Lopez, Urban Regional Planner

Email: JELopez@rivco.org

Address: 4080 Lemon St 12th Floor Riverside, CA 92501

Local Agency
Case No.:

PP20682R01 (revision to previously approved plot plan)

Project Location

Street
Address:

N/A

Gross Parcel Size: 10.72 AC

Assessor's Parcel No.: A portion of APN 480-170-012

Solar

Is the project proposing solar Panels? Yes

No

If yes, please provide solar glare study.
(only if in Zone C or higher)

Data

Site Elevation:(above mean sea level) 1350'

Height of Building or structures: 36'-6" MAX.

What type of drainage basins are being proposed and the square footage: BIORETENTION BASINS (2): 3,840 SF. & UNDERGROUND CHAMBERS (2): 21,770 SF.

Notice

A. NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. REVIEW TIME: Estimated time for “staff level review” is approximately 30 days from date of submittal. Estimated time for “commission level review” is approximately 45 days from date of a complete application submittal to the next available commission hearing meeting.

C. SUBMISSION PACKAGE:

Please submit all application items DIGITALLY via USB or CD:

- Completed ALUC Application Form
- Plans Package: site plans, floor plans, building elevations, grading plans, subdivision maps
- Exhibits of change of zone, general plan amendment, specific plan amendment
- Project description of existing and proposed use

Additionally, please provide:

- ALUC fee payment (Checks made out to Riverside County ALUC)
- Gummed address labels of all surrounding property owners within a 300-foot radius of project site. (Only required if the project is scheduled for a public hearing).

SCHEDULE OF DEVELOPMENT REVIEW FEES (effective 3/1/19)

CASE TYPE	ALL OTHERS		MARCH ZONE E	
	INITIAL REVIEW FEE	AMENDED REVIEW FEE	INITIAL REVIEW FEE	AMENDED REVIEW FEE
General Plan or General Plan Element (County or City)	\$3,696	\$2,458	\$2,310	\$1,537
Community Plan or Area Plan (County or City)	\$3,696	\$2,402	\$2,310	\$1,502
(New) Specific Plan or Master Plan	\$3,261	N/A	\$2,038	N/A
Specific Plan Amendment	N/A	\$2,181	N/A	\$1,363
General Plan Amendment	\$1,331	N/A	\$832	N/A
Change of Zone or Ordinance Amendment	\$1,331	\$887	\$832	\$554
Non-Impact Legislative Project (as determined by staff)	\$420	N/A	\$375	N/A
Tract Map	\$1,515	\$1,017	\$947	\$636
Conditional Use Permit or Public Use Permit	\$1,331	\$887	\$832	\$554
Plot Plan, Development Review Plan or Design Review	\$1,331	\$887	\$832	\$554
Parcel Map	\$1,331	\$887	\$832	\$554
Environmental Impact Report*	\$3,050	\$2,033	\$1,906	\$1,271
Other Environmental Assessments*	\$1,671	\$1,109	\$1,044	\$693
Building Permit or Tenant Improvement	\$573	\$389	\$359	\$243

Effective March 1, 2019, an additional fee of \$190.00 will be charged to projects requiring ALUC public hearings (no additional fee for staff review cases).

ADDITIONAL PROJECT SPECIFIC FEES (in addition to the above fees)				
Location in APZ I or II of March	\$2,500	\$2,500	N/A	N/A
AIA Large Commercial Solar Project (Energy Generation Facility)	\$3,000	\$3,000	\$3,000	\$3,000
Heliports/Helicopter Landing Sites	\$1,000	\$1,000	\$1,000	\$1,000
Speculative Nonresidential Multiple Buildings (4 or more)	\$8,210	\$8,210	N/A	N/A

NOTE: * THIS FEE IS COLLECTED ONLY FOR PROJECTS THAT ARE NOT CLASSIFIED UNDER ONE OF THE ABOVE CATEGORIES.

Checks should be made payable to: Riverside County Airport Land Use Commission

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM: 3.4

HEARING DATE: June 8, 2023

CASE NUMBER: ZAP1029PV23 – IDI Logistics (Representative: T&B Planning)

APPROVING JURISDICTION: City of Perris

JURISDICTION CASE NO: PLN22-05265 (Major Modification Review), TPM38518 (Tentative Parcel Map).

LAND USE PLAN: 2011 Perris Valley Airport Land Use Compatibility Plan, 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

Airport Influence Area: Perris Valley Airport, March Air Reserve Base/Inland Port Airport

Land Use Policy: Zones D and E (Perris Valley); Zone E (March)

Noise Levels: Between 65 - 70 CNEL range from Perris Valley aircraft; Below 60 CNEL from March aircraft

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends that the Commission find the Major Modification Review and Tentative Parcel Map CONSISTENT, with the 2011 Perris Valley Airport Land Use Compatibility Plan and the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, subject to the conditions included herein.

PROJECT DESCRIPTION: A proposal to construct three industrial warehouse buildings totaling 3,343,584 square feet on three parcels on 166.36 total acres. The applicant also proposes to divide the parcel into three commercial parcels.

PROJECT LOCATION: The site is located southerly of Mapes Street, westerly of Goetz Road, and easterly of A Street, approximately 2,089 feet southwesterly of the southerly terminus of Runway 15-33 at Perris Valley Airport.

BACKGROUND:

Non-Residential Average-Acre Intensity: Pursuant to the Perris Valley Airport Land Use Compatibility Plan, the project is located within Zones D (131.07 acres) and E (60.695 acres), which limits average intensity to 100 people per average acre in Zone D, and Zone E does not restrict non-residential intensity. The project is also located in Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, which does not restrict non-residential intensity.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, the following rates were used to calculate the occupancy for the proposed buildings:

- Office – 1 person per 200 square feet
- Warehouse – 1 person per 500 square feet

The project proposes to construct 3 industrial buildings totaling 3,343,584 square feet (on separate parcels) on a total of 166.36 acres. An individual lot-by-lot analysis is included below:

- Building 1, Parcel 1 (65.17 acres) is located within Zones D (10.04 acres) and Zone E (55.12 acres) and proposes a 1,385,090 square foot warehouse building, which includes 492,883 square feet of warehouse area in Zone D, accommodating an occupancy of 986 people, and resulting in an average intensity of 98 people per acre, which is consistent with the Compatibility Zone D average intensity criterion of 100 people per acre. The portion of the building located within Zone E includes 872,207 square feet of warehouse area, and 20,000 square feet of office area, which accommodates an occupancy of 1,844 people, and resulting in an average intensity of 34 people per acre, which is consistent with Zone E intensity criteria which is not restricted.
- Building 2, Parcel 2 (64.09 acres) is located within Zone D (58.52 acres) and Zone E (5.57 acres) and proposes a 1,424,920 square foot warehouse building, which includes 1,313,113 square feet of warehouse area, and 1,500 square feet of office area in Zone D, accommodating an occupancy of 2,634 people, and resulting in an average intensity of 45 people per acre, which is consistent with the Compatibility Zone D average intensity criterion of 100 people per acre. The portion of the building located within Zone E consist of 105,307 square feet of warehouse area, and 5,000 square feet of office area, which accommodates an occupancy of 236 people, resulting in an average intensity of 42 people per acre, which is consistent with Zone E intensity criteria which is not restricted.
- Building 3, Parcel 3 (37.10 acres) is located within Zones D, and proposes a 533,574 square foot warehouse building, which includes 523,574 square feet of warehouse area, and 10,000 square feet of office area, accommodating an occupancy of 1,097 people, and resulting in an average intensity of 30 people per acre, which is consistent with the Compatibility Zone D average intensity criterion of 100 people per acre.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle and 1.0 persons per trailer truck space). An individual lot-by-lot analysis is included below:

- Building 1 Parcel 1 includes 238 standard vehicles and 339 trailer spaces in Zone D, accommodating a total occupancy of 696 people, resulting in an average intensity of 69

people per acre, which is consistent with the Compatibility Zone D average intensity criterion of 100 people per acre. The portion of the project located within Zone E includes 295 standard vehicles, accommodating a total occupancy of 443 people, resulting in an average intensity of 8 people per acre, which is consistent with Zone E intensity criteria which is not restricted.

- Building 2 Parcel 2 includes 310 standard vehicles and 316 trailer spaces in Zone D, accommodating a total occupancy of 781 people, resulting in an average intensity of 13 people per acre, which is consistent with the Compatibility Zone D. The portion of the project located within Zone E includes 88 standard vehicles, accommodating a total occupancy of 132 people, resulting in an average intensity of 24 people per acre, which is consistent with Zone E intensity criteria which is not restricted.
- Building 3 Parcel 3 includes 271 standard vehicles and 131 trailer spaces, accommodating a total occupancy of 538 people, resulting in an average intensity of 15 people per acre, which is consistent with the Compatibility Criteria for Zone D average intensity criterion of 100 people per acre.

Non-Residential Single-Acre Intensity: Pursuant to the Perris Valley Airport Land Use Compatibility Plan, the project boundary is located within Zones D (131.07 acres) and E (60.69 acres), which limits single acre intensity to 300 people in Zone D, and Zone E intensity is not restricted. The project is also located within March Air Reserve Base/Inland Port Airport Zone E, which does not restrict non-residential intensity.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre area for each of the buildings are as follows:

- Building 1 single acre intensity located in Zone D includes 43,560 square feet of warehouse building, resulting in single acre intensity of 87 people, consistent with the Compatibility Zone D single acre intensity criterion maximum of 300 people.
- Building 2 single acre intensity located in Zone D includes 38,560 square feet of warehouse area, and 5,000 square feet of office area, resulting in a single acre intensity of 102 people, which is consistent with the Compatibility Zone D single acre intensity criterion maximum of 300 people.
- Building 3 single acre intensity located in Zone D includes 33,560 square feet of warehouse area, and 10,000 square feet of office area, resulting in a single acre intensity of 117 people, which is consistent with the Compatibility Zone D single acre intensity criterion maximum of 300 people.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zones D or E.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being in an area below the 60 CNEL range from aircraft noise. The Perris Valley Airport Land Use Compatibility Plan depicts the site as being affected by aircraft noise of 65 - 70 CNEL. As a primarily industrial use not sensitive to noise (and considering typical anticipated building

construction noise attenuation of approximately 20 dBA), the warehouse area would not require special measures to mitigate aircraft-generated noise. However, a condition is included to provide for adequate noise attenuation within office areas of the buildings.

Part 77: The elevation of Perris Valley Airport's Runway 15-33 at its southeasterly terminus is 1,413 feet above mean sea level (AMSL). At a distance of approximately 2,089 feet from the runway to the site, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,434 feet AMSL. The maximum finished floor elevation is 1,424 feet AMSL and the maximum building height is 52 feet, resulting in a top point elevation of 1,476 feet AMSL. Therefore, review of this building by the FAA Obstruction Evaluation Service (FAA OES) is required. The applicant has submitted Form 7460-1, and FAA OES has assigned Aeronautical Study Nos. 2023-AWP-4892-OE thru 2023-AWP-4895-OE, 2023-AWP-4888-OE thru 2023-AWP-4891-OE, and 2023-AWP-4884-OE thru 2023-AWP-4887-OE to this project.

Determinations of No Hazard to Air Navigation letters were issued by the FAA OES and it was determined that the buildings would not result in an impact to air navigation. The FAA OES conditions have been incorporated into ALUC's conditions.

Open Area: The project site is split between Compatibility Zones D (131.07 acres) and E (60.69 acres). Compatibility Zone D requires 10% (13.10 acres) of the land area within major projects (10 acres or larger) be set aside as open area that could potentially serve as emergency landing areas (Compatibility Zone E does not require any amount of open area). The project provides 13.10 acres of ALUC eligible open areas consisting of driveway aisles and parking lot areas within the proposed development. The project is conditioned to maintain these areas consistent with ALUC open area requirements of 300 feet by 75 feet minimum shape and prohibit obstructions greater than 4 feet in height that are at least 4 inches in diameter.

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33C). The project is located 2,089 feet from the runway, and therefore would be subject to the above requirement.

The project includes a 126,759 square foot bioretention basin. Bioretention basins are not recommended in the vicinity of airports due to the potential that such areas could provide food, water, and shelter for hazardous wildlife. Pursuant to the study "Wildlife Hazard Management at Riverside County Airports: Background and Policy", October 2018, by Mead & Hunt, which is the basis of the brochure titled "Airports, Wildlife and Stormwater Management", such limited basins are permissible with the appropriate criteria: the basin is used in conjunction with appropriate landscaping for such uses as adjacent to structures, parking islands, medians, site entrances, planter boxes, and vegetation is selected carefully so as not to provide food, shelter, nesting, roosting, or water for wildlife. The project has been conditioned to be consistent with the basin criteria (as well as providing 48-hour draw down of the basin).

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight or circling climb following takeoff or toward an aircraft engaged in a straight or circling final approach toward a landing at an airport, other than a DoD or FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight or circling climb following takeoff or towards an aircraft engaged in a straight or circling final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor, or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Hazards to flight
3. The attached notice shall be provided to all prospective purchasers of the property and tenants of the building, and shall be recorded as a deed notice.
4. Any proposed detention basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the detention basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

5. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
6. The project has been evaluated to construct three industrial buildings totaling 3,343,584 square feet on three separate parcels. Any increase in building area, change in use to any higher intensity use, change in building location, or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.
7. Noise attenuation measures shall be incorporated into the design of the office area, to the extent such measures are necessary to ensure that interior noise levels from aircraft operations are at or below 45 CNEL.
8. At least 13.10 acres of ALUC-eligible open areas (at least 75 feet in width and 300 feet in length), as depicted on the Open Space exhibit, shall be kept obstacle and obstruction free per ALUC open area definition (no objects greater than four feet in height with a diameter of four inches or greater).
9. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission.
10. The Federal Aviation Administration has conducted aeronautical studies of the proposed project (Aeronautical Study Nos. 2023-AWP-4892-OE thru 2023-AWP-4895-OE, 2023-AWP-4888-OE thru 2023-AWP-4891-OE, and 2023-AWP-4884-OE thru 2023-AWP-4887-OE) and has determined that the buildings would not result in an impact to air navigation.
11. The proposed buildings and site elevations shall not exceed the heights identified in the aeronautical studies.
12. The maximum height and top point elevation specified above shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in structure height or elevation shall not require further review by the Airport Land Use Commission.
13. Temporary construction equipment used during actual construction of the structure(s) shall not exceed the structure heights and site elevations as identified in the aeronautical studies, unless separate notice is provided to the Federal Aviation Administration through the Form

7460-1 process.

14. Within five (5) days after construction of the proposed building reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned or a decision is made not to construct the applicable structure.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS

PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-4892-OE

Issued Date: 03/13/2023

Brandon Dickens
 IDI Logistics
 840 Apollo Street, Suite 343
 El Segundo, CA 90245

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building Perris Logistic Center - Building 1
 Location: Perris, CA
 Latitude: 33-45-22.83N NAD 83
 Longitude: 117-13-51.83W
 Heights: 1432 feet site elevation (SE)
 52 feet above ground level (AGL)
 1484 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 09/13/2024 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-4892-OE.

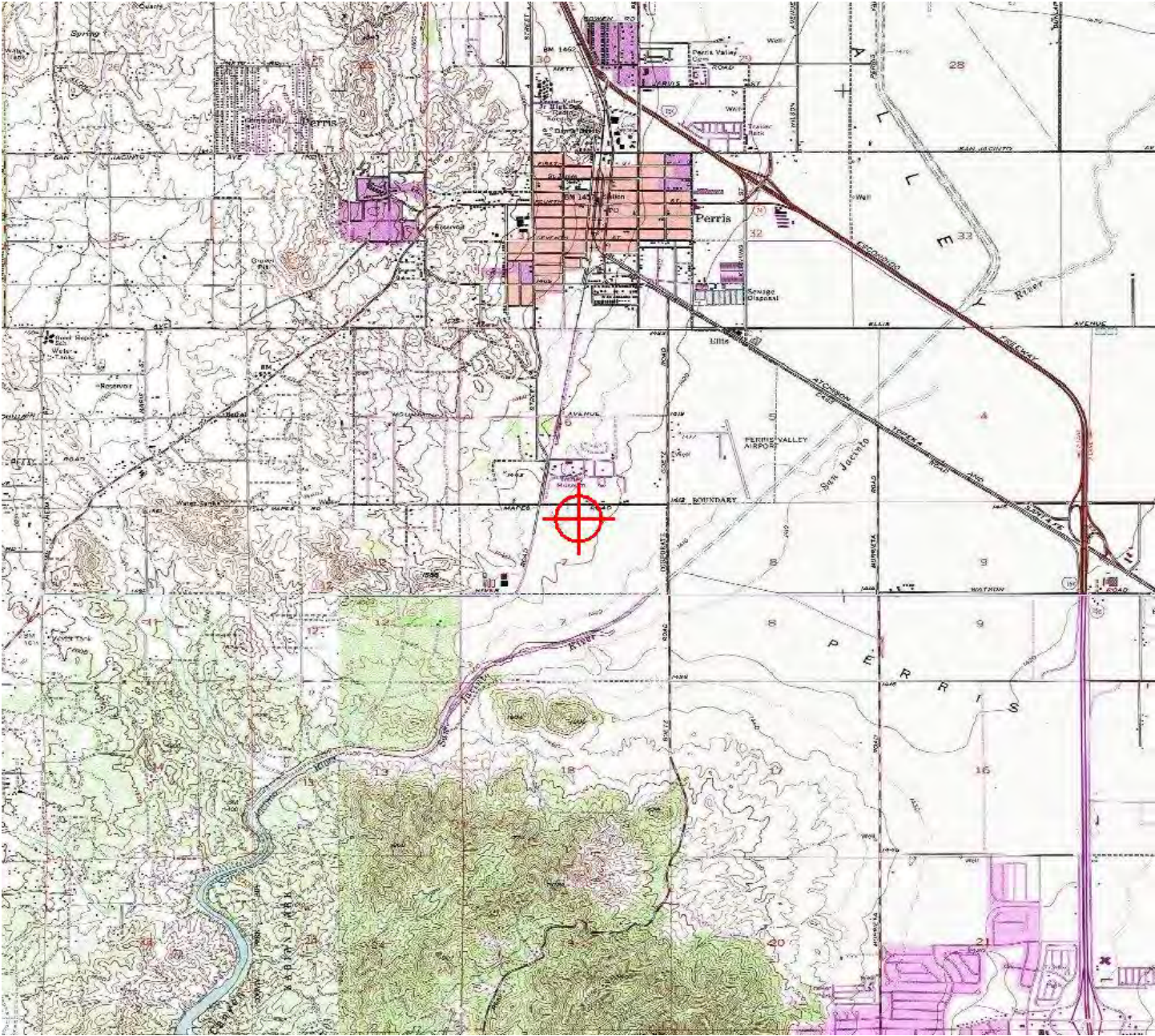
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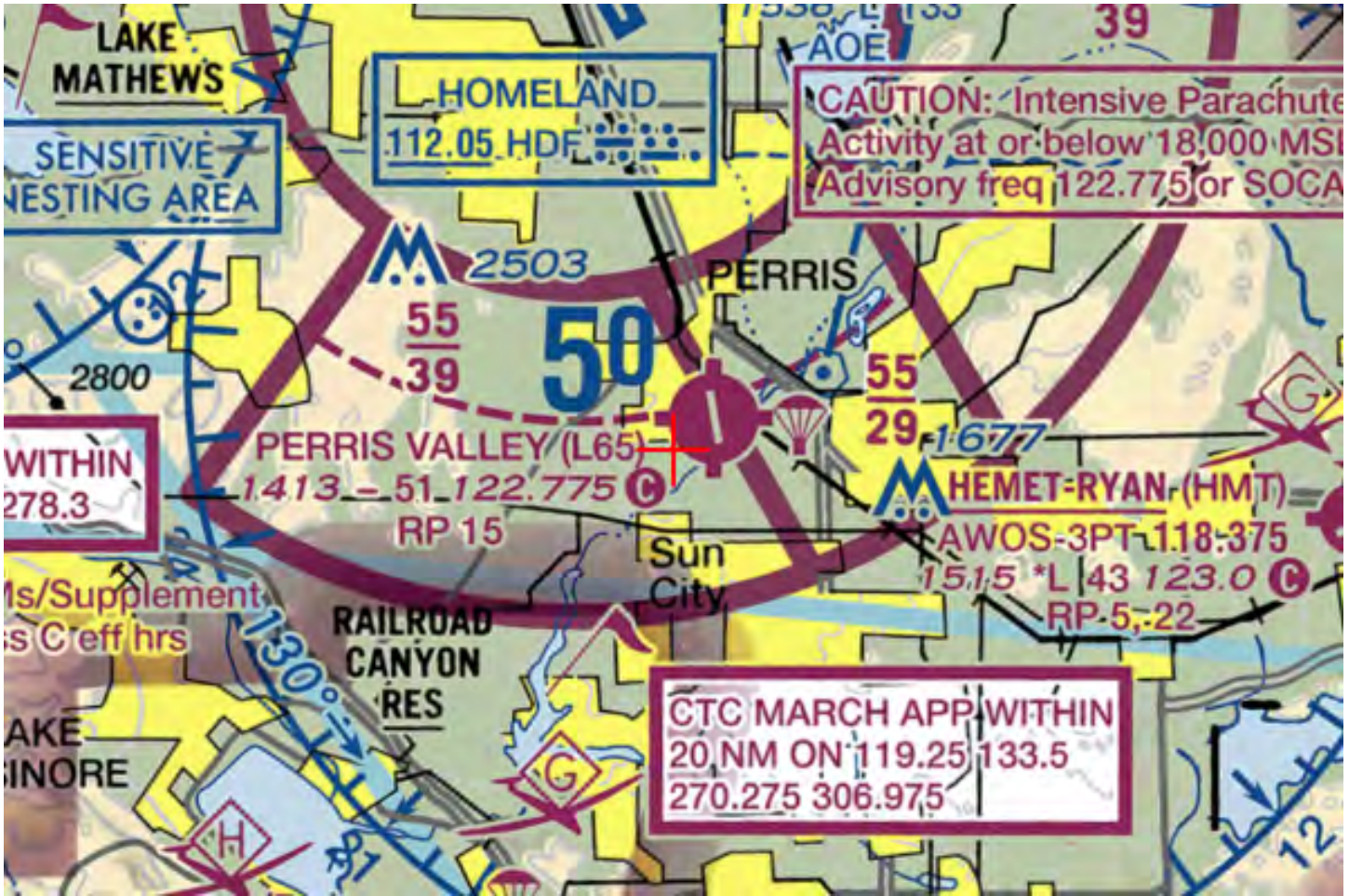
(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-4892-OE







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-4893-OE

Issued Date: 03/13/2023

Brandon Dickens
 IDI Logistics
 840 Apollo Street, Suite 343
 El Segundo, CA 90245

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building Perris Logistic Center - Building 1
 Location: Perris, CA
 Latitude: 33-45-24.13N NAD 83
 Longitude: 117-14-00.45W
 Heights: 1430 feet site elevation (SE)
 52 feet above ground level (AGL)
 1482 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 09/13/2024 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-4893-OE.

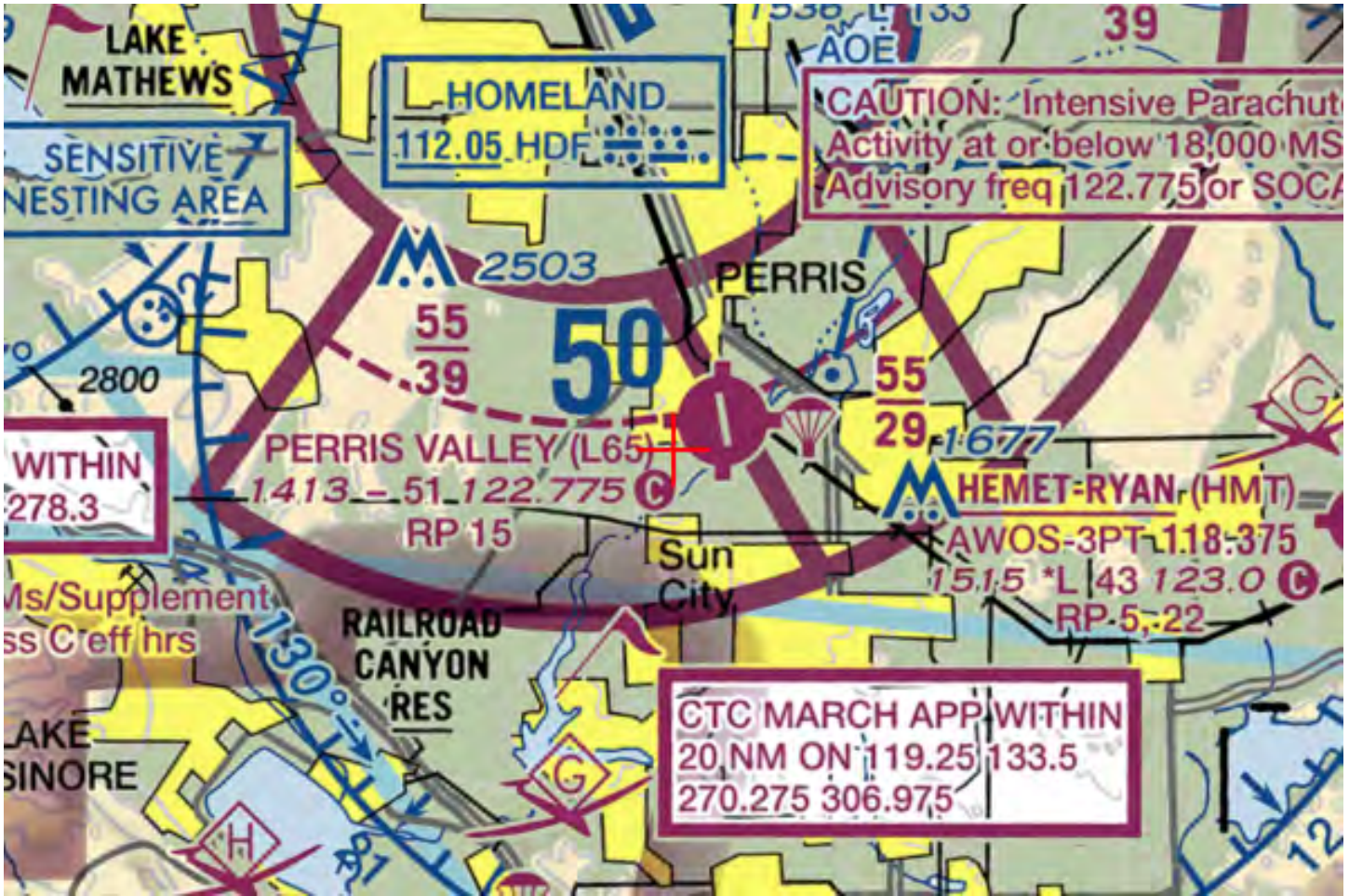
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(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-4894-OE

Issued Date: 03/13/2023

Brandon Dickens
 IDI Logistics
 840 Apollo Street, Suite 343
 El Segundo, CA 90245

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building Perris Logistic Center - Building 1
 Location: Perris, CA
 Latitude: 33-45-03.96N NAD 83
 Longitude: 117-13-56.46W
 Heights: 1420 feet site elevation (SE)
 52 feet above ground level (AGL)
 1472 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 09/13/2024 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

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This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-4894-OE.

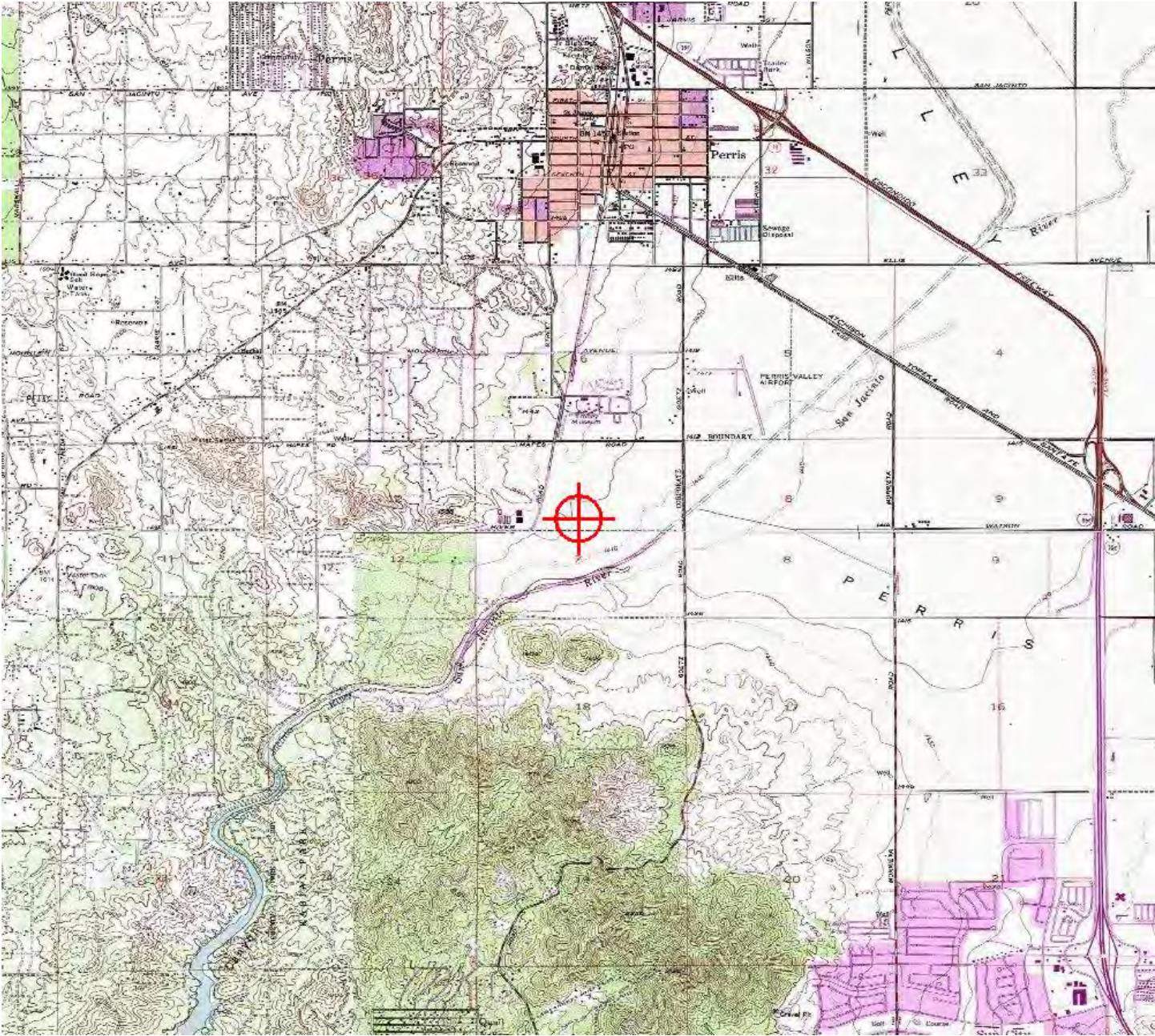
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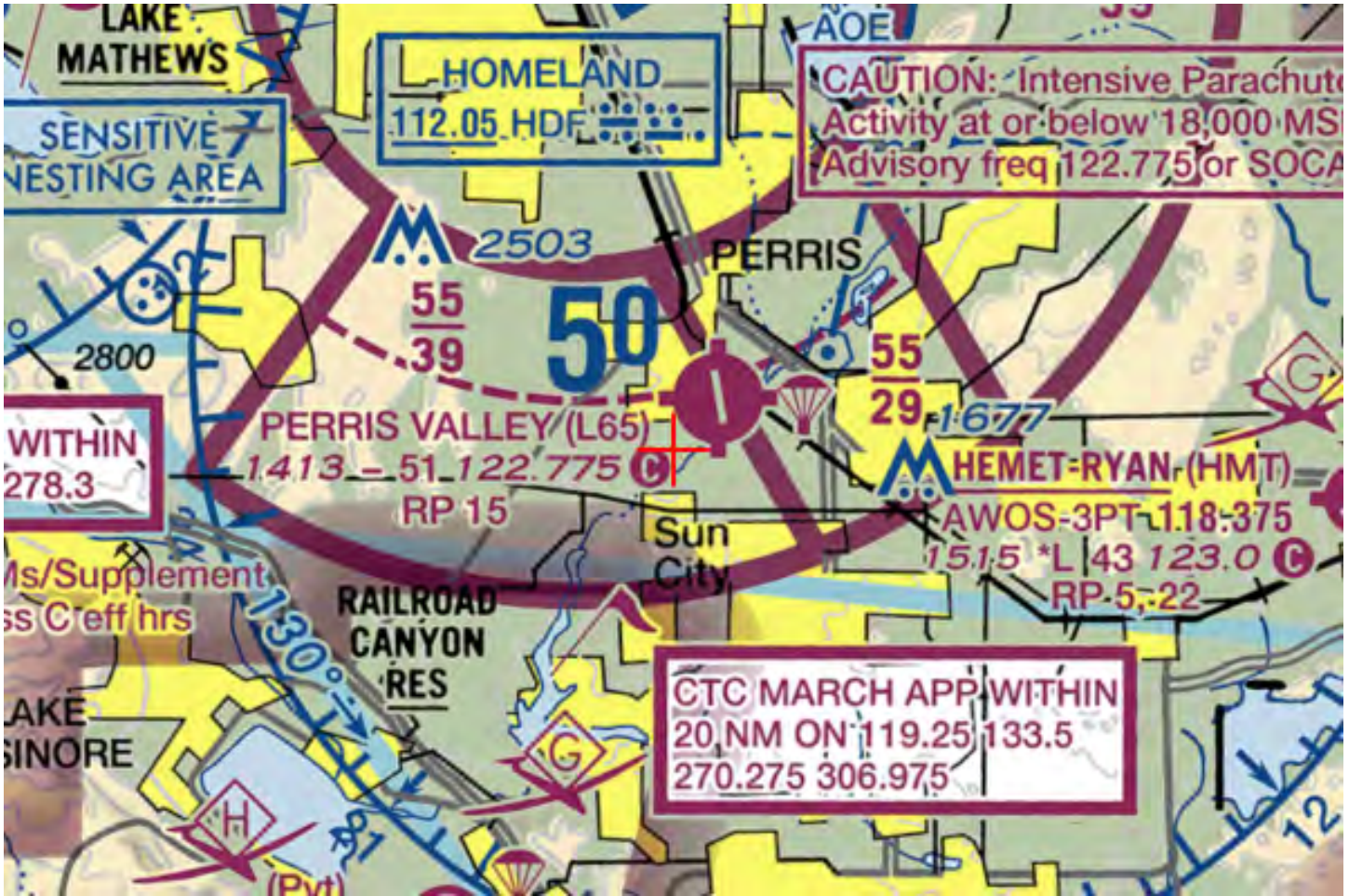
(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-4894-OE







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-4895-OE

Issued Date: 03/13/2023

Brandon Dickens
 IDI Logistics
 840 Apollo Street, Suite 343
 El Segundo, CA 90245

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building Perris Logistic Center - Building 1
 Location: Perris, CA
 Latitude: 33-45-04.46N NAD 83
 Longitude: 117-14-04.56W
 Heights: 1425 feet site elevation (SE)
 52 feet above ground level (AGL)
 1477 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 09/13/2024 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-4895-OE.

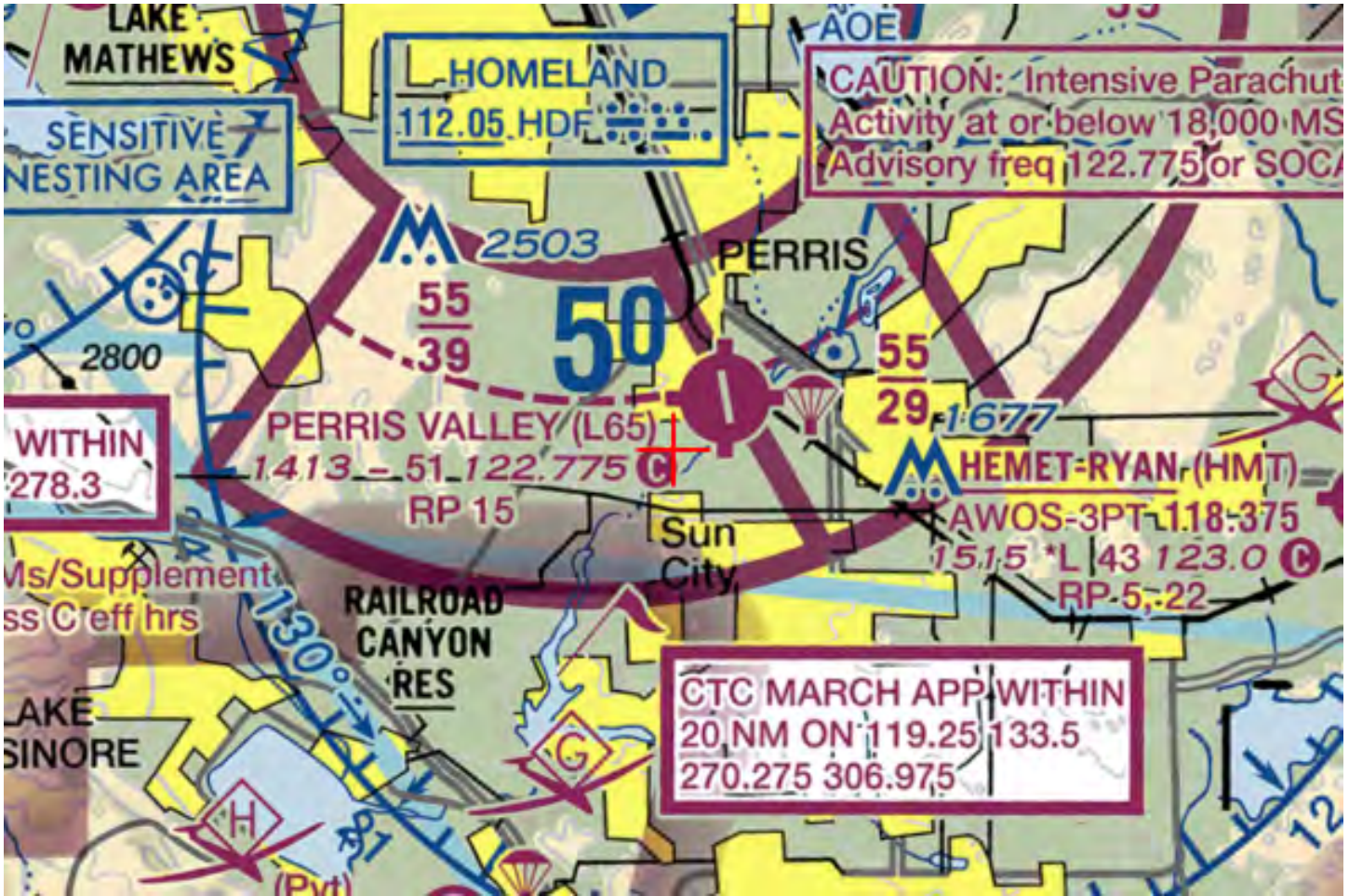
Signature Control No: 575193372-576061952

(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-4888-OE

Issued Date: 03/13/2023

Brandon Dickens
 IDI Logistics
 840 Apollo Street, Suite 343
 El Segundo, CA 90245

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building Perris Logistic Center - Building 2
 Location: Perris, CA
 Latitude: 33-45-23.25N NAD 83
 Longitude: 117-13-38.51W
 Heights: 1420 feet site elevation (SE)
 52 feet above ground level (AGL)
 1472 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 09/13/2024 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-4888-OE.

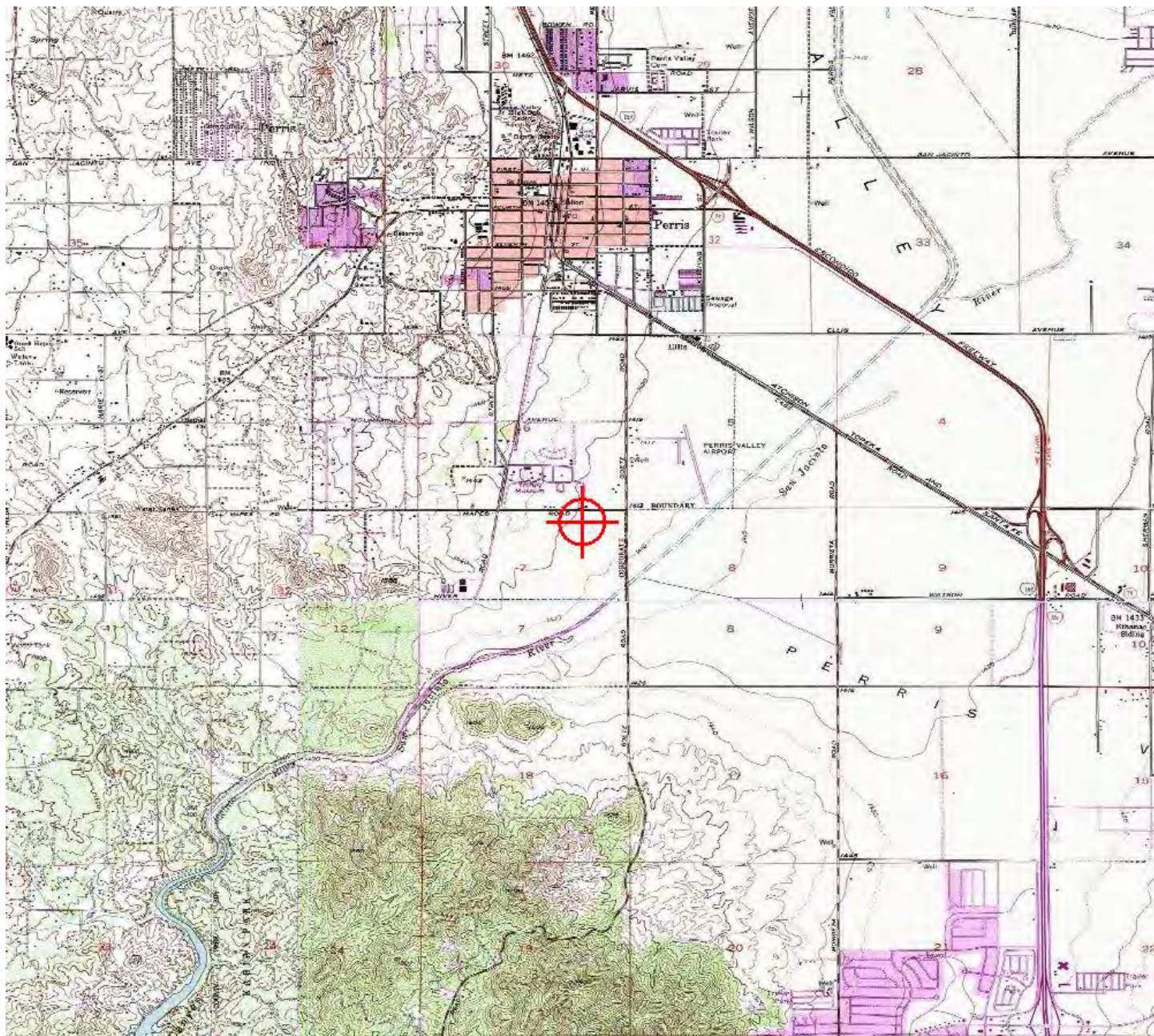
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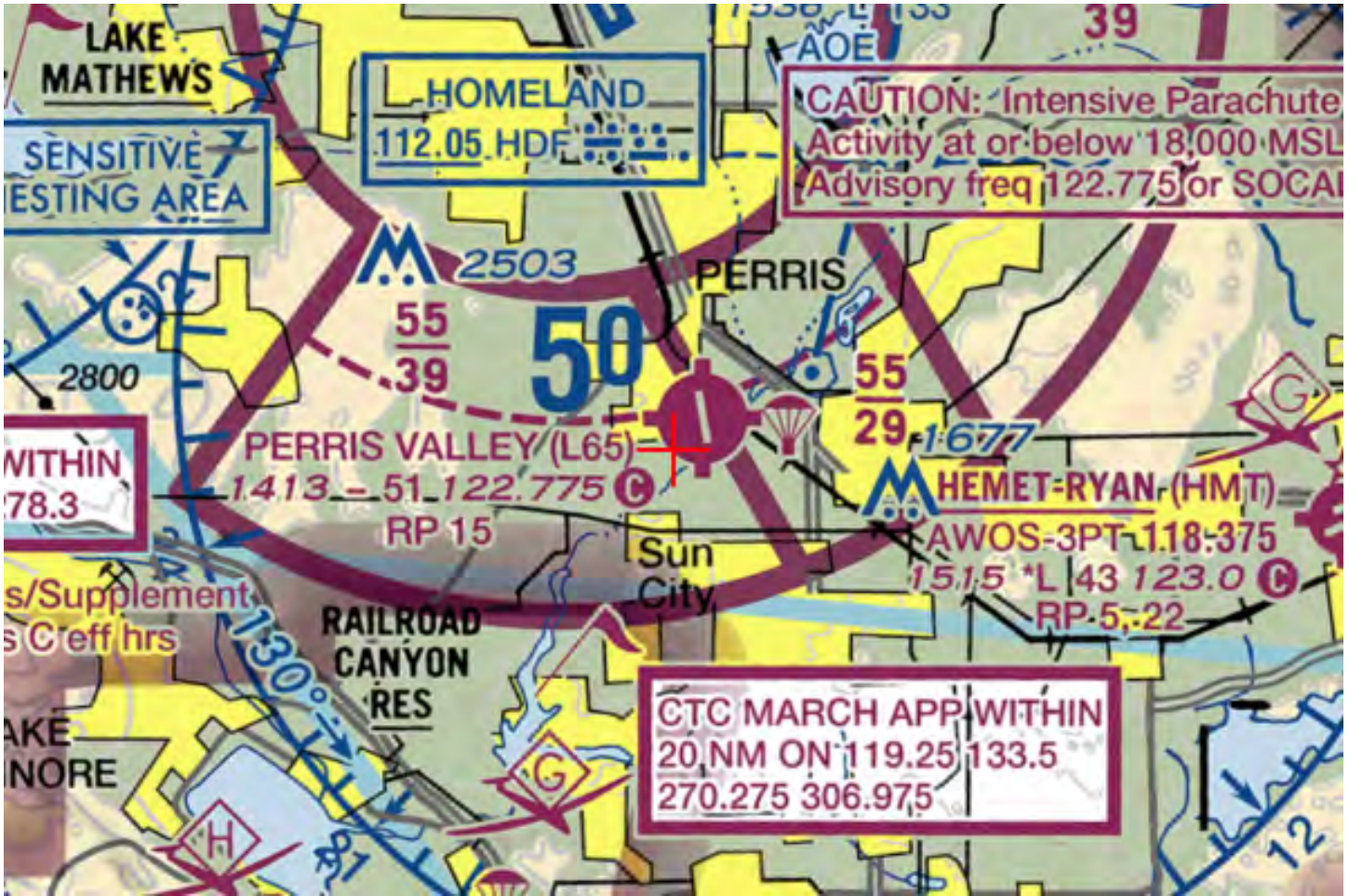
(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-4888-OE







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-4889-OE

Issued Date: 03/13/2023

Brandon Dickens
 IDI Logistics
 840 Apollo Street, Suite 343
 El Segundo, CA 90245

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building Perris Logistic Center - Building 2
 Location: Perris, CA
 Latitude: 33-45-24.64N NAD 83
 Longitude: 117-13-47.39W
 Heights: 1427 feet site elevation (SE)
 52 feet above ground level (AGL)
 1479 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 09/13/2024 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-4889-OE.

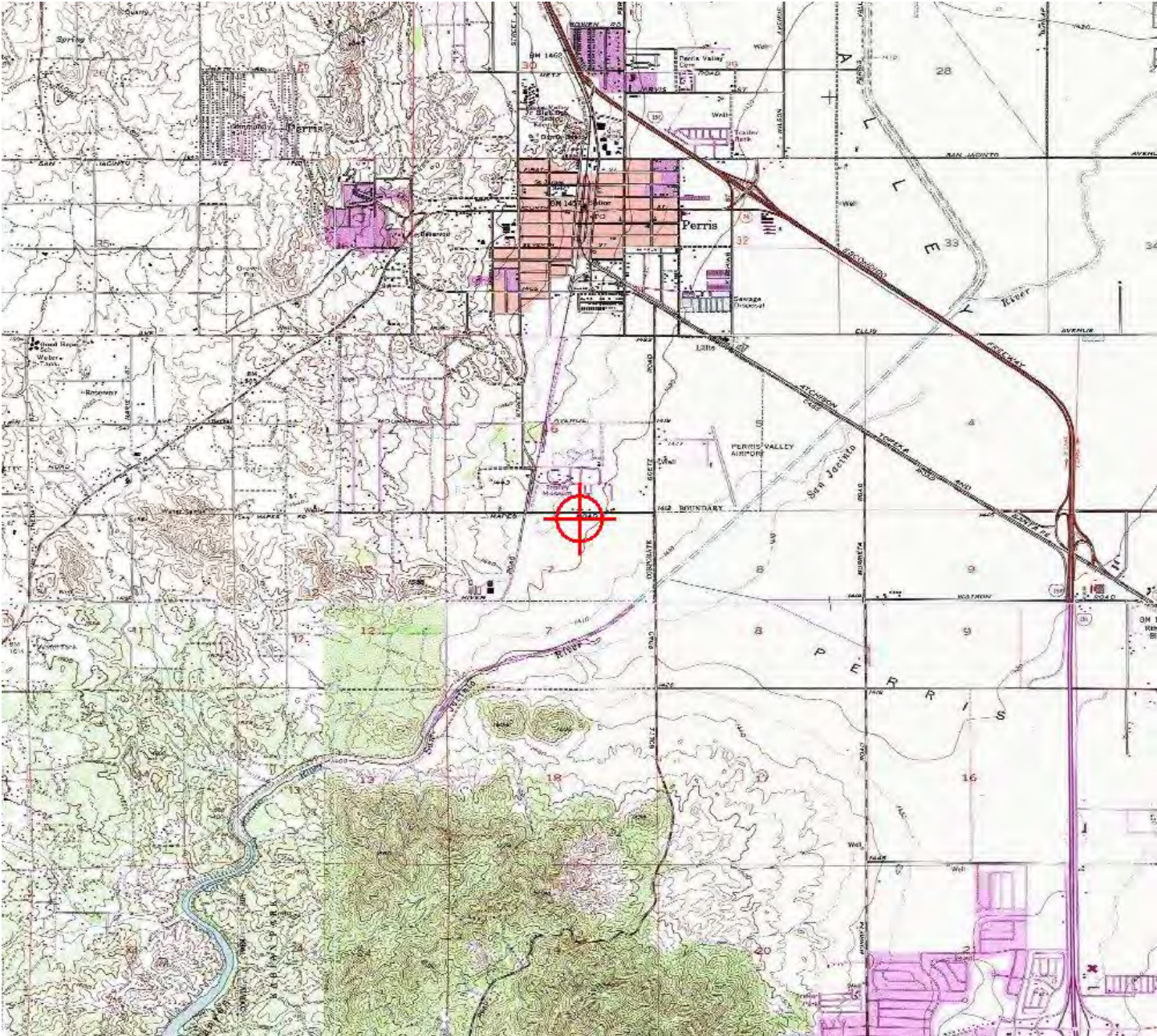
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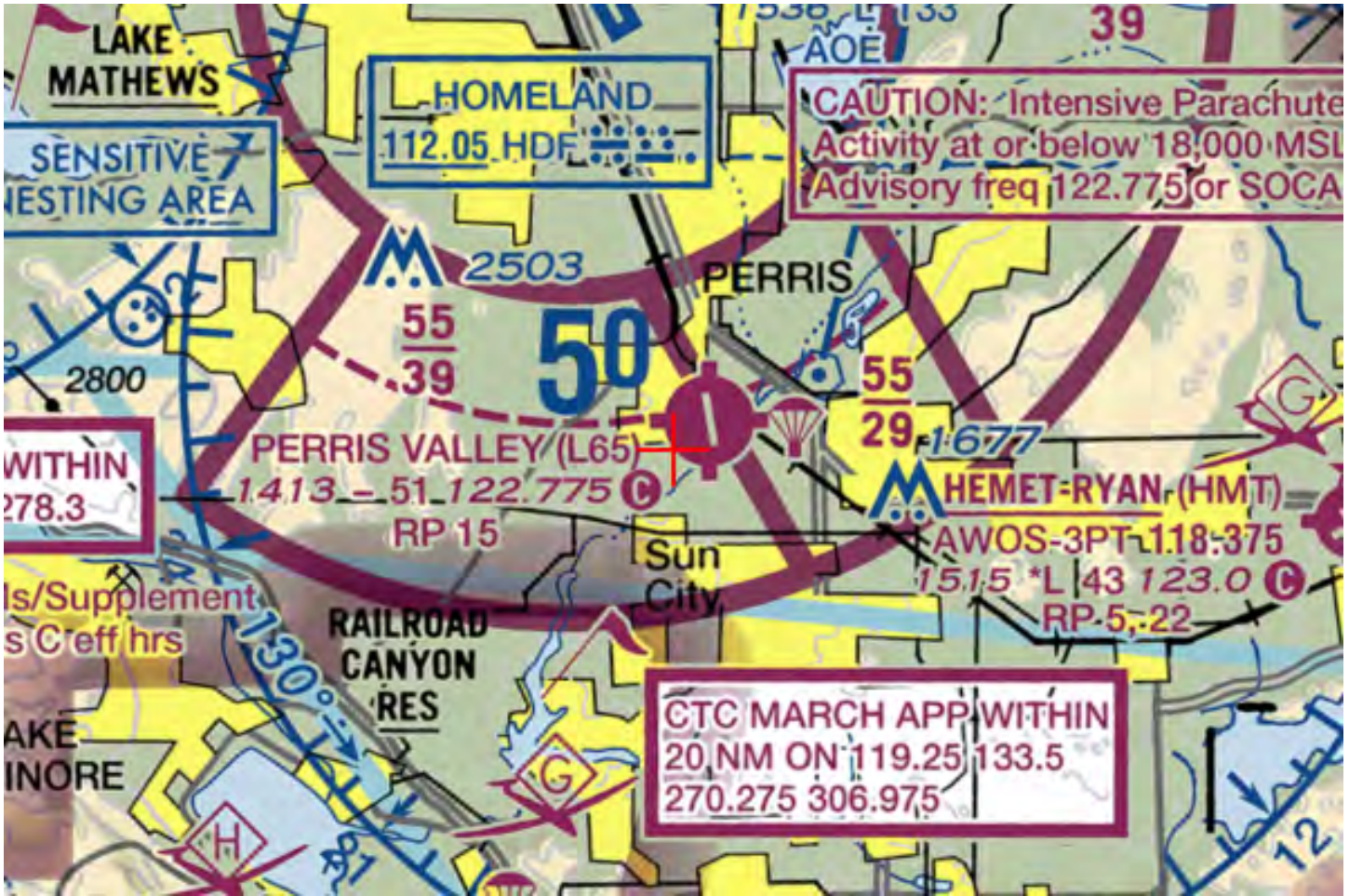
(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-4889-OE







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-4890-OE

Issued Date: 03/13/2023

Brandon Dickens
 IDI Logistics
 840 Apollo Street, Suite 343
 El Segundo, CA 90245

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building Perris Logistic Center - Building 2
 Location: Perris, CA
 Latitude: 33-45-04.06N NAD 83
 Longitude: 117-13-42.62W
 Heights: 1415 feet site elevation (SE)
 52 feet above ground level (AGL)
 1467 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 09/13/2024 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

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This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-4890-OE.

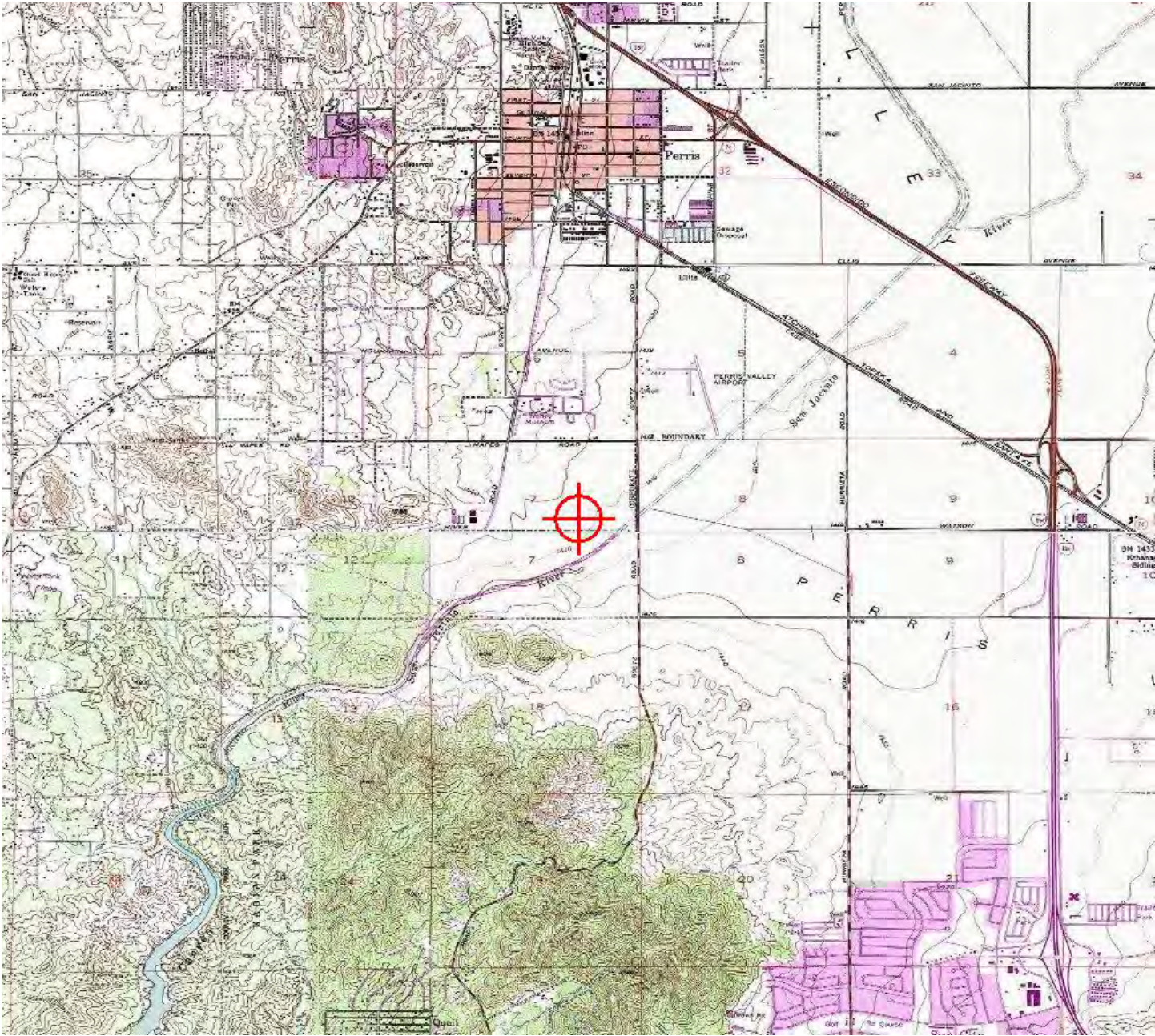
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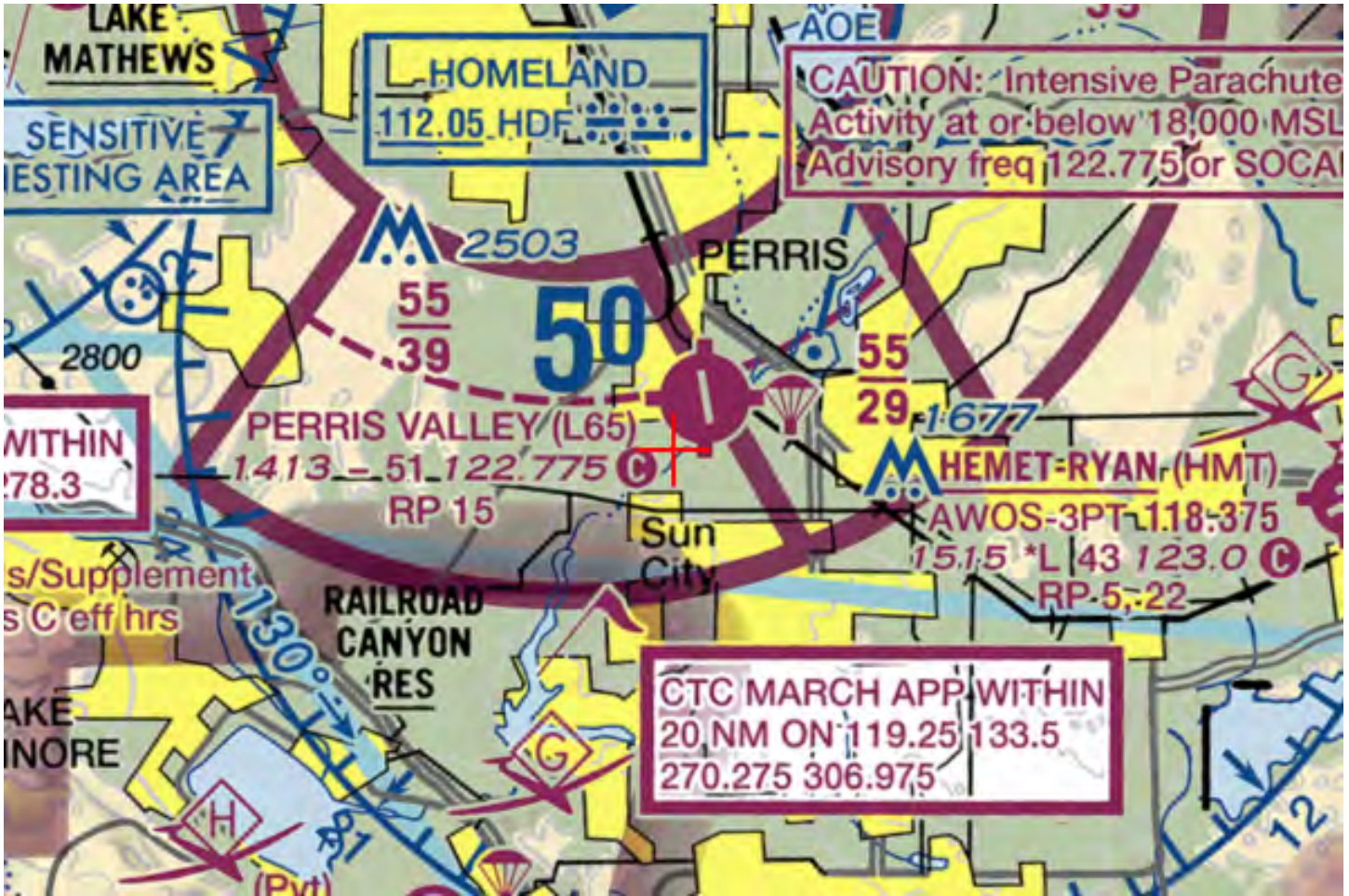
(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-4890-OE







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-4891-OE

Issued Date: 03/13/2023

Brandon Dickens
 IDI Logistics
 840 Apollo Street, Suite 343
 El Segundo, CA 90245

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building Perris Logistic Center - Building 2
 Location: Perris, CA
 Latitude: 33-45-04.64N NAD 83
 Longitude: 117-13-51.33W
 Heights: 1420 feet site elevation (SE)
 52 feet above ground level (AGL)
 1472 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 09/13/2024 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-4891-OE.

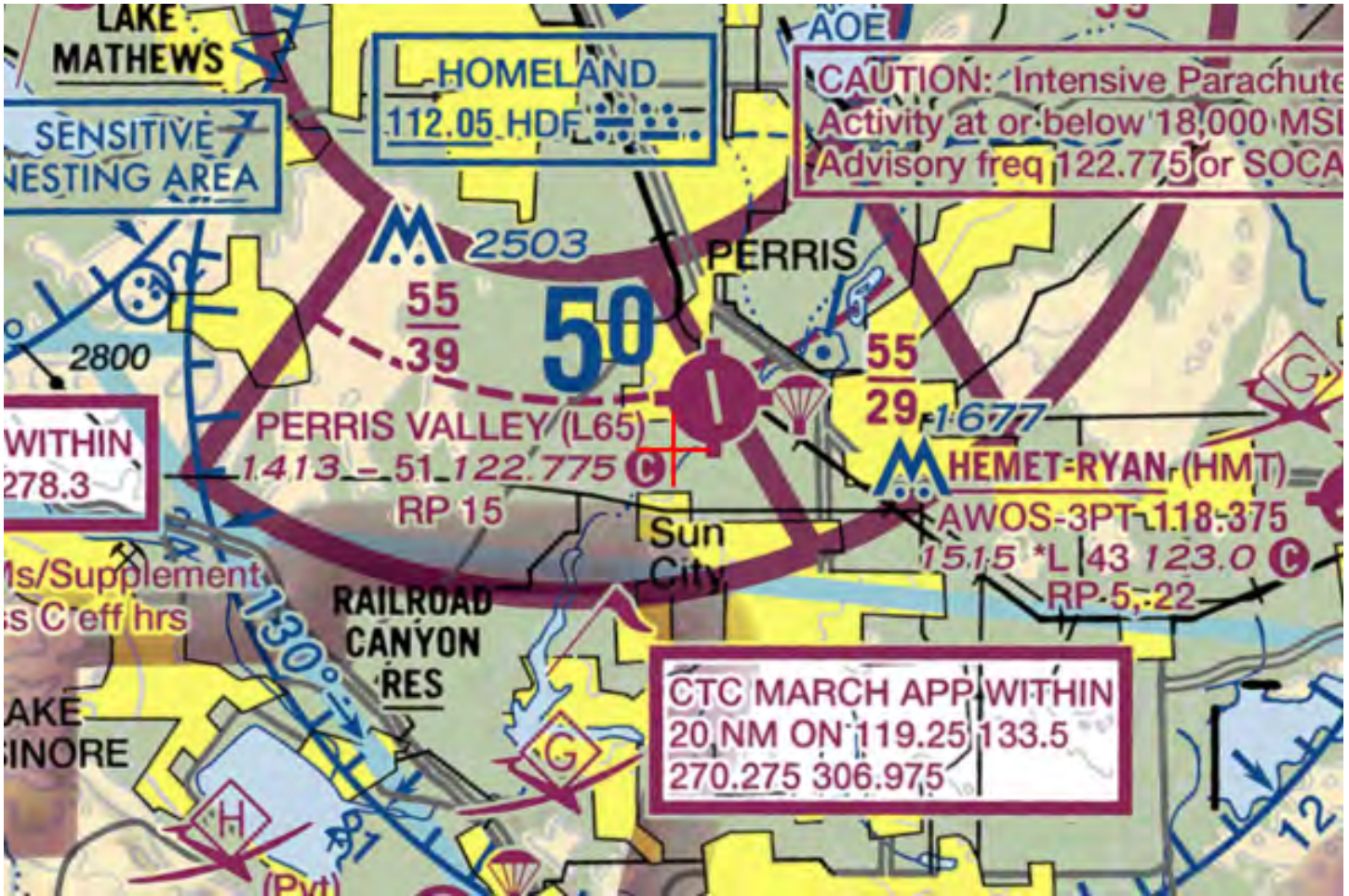
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(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-4884-OE

Issued Date: 03/13/2023

Brandon Dickens
 IDI Logistics
 840 Apollo Street, Suite 343
 El Segundo, CA 90245

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building Perris Logistic Center - Building 3
 Location: Perris, CA
 Latitude: 33-45-23.54N NAD 83
 Longitude: 117-13-28.24W
 Heights: 1415 feet site elevation (SE)
 52 feet above ground level (AGL)
 1467 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 09/13/2024 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-4884-OE.

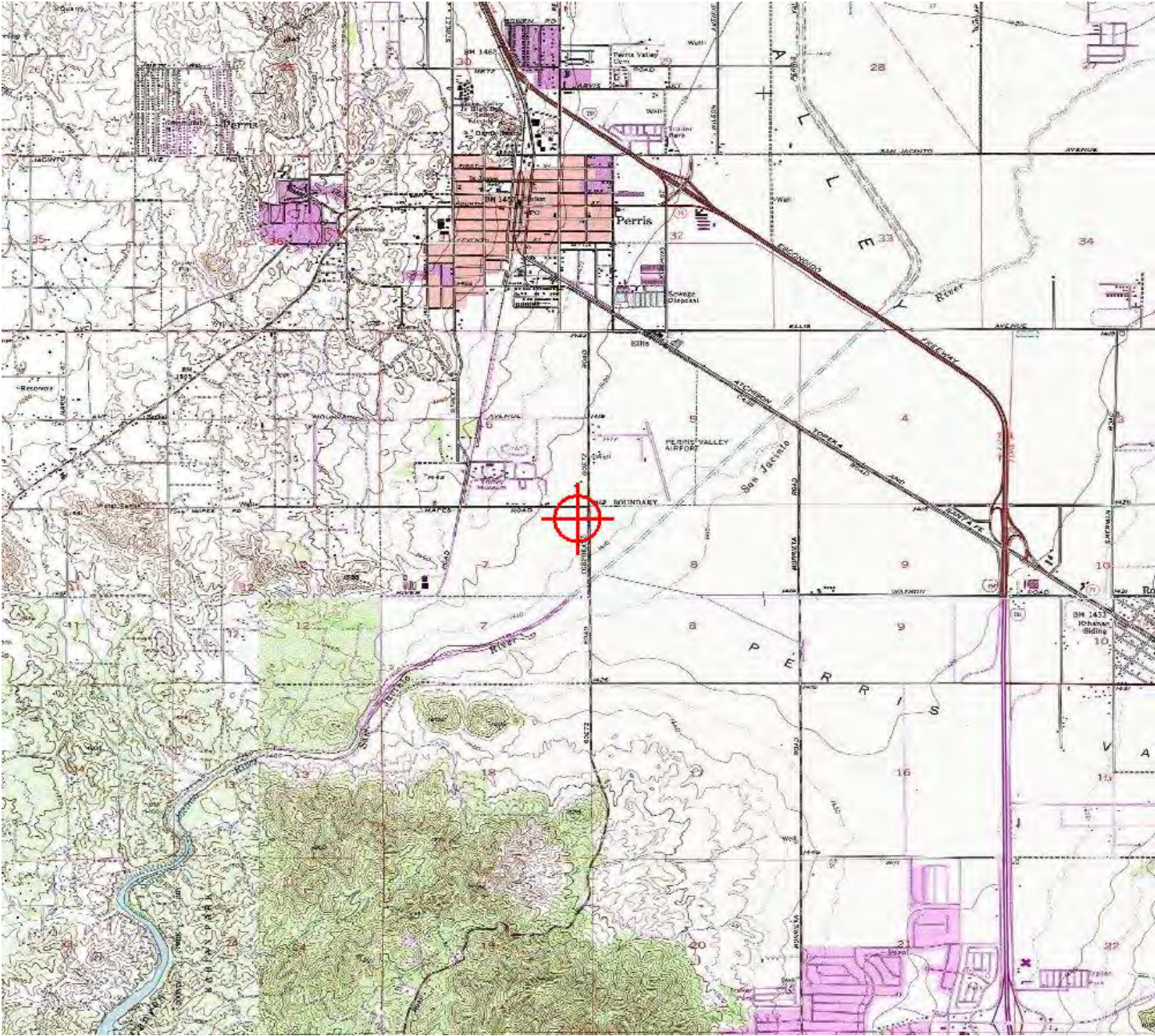
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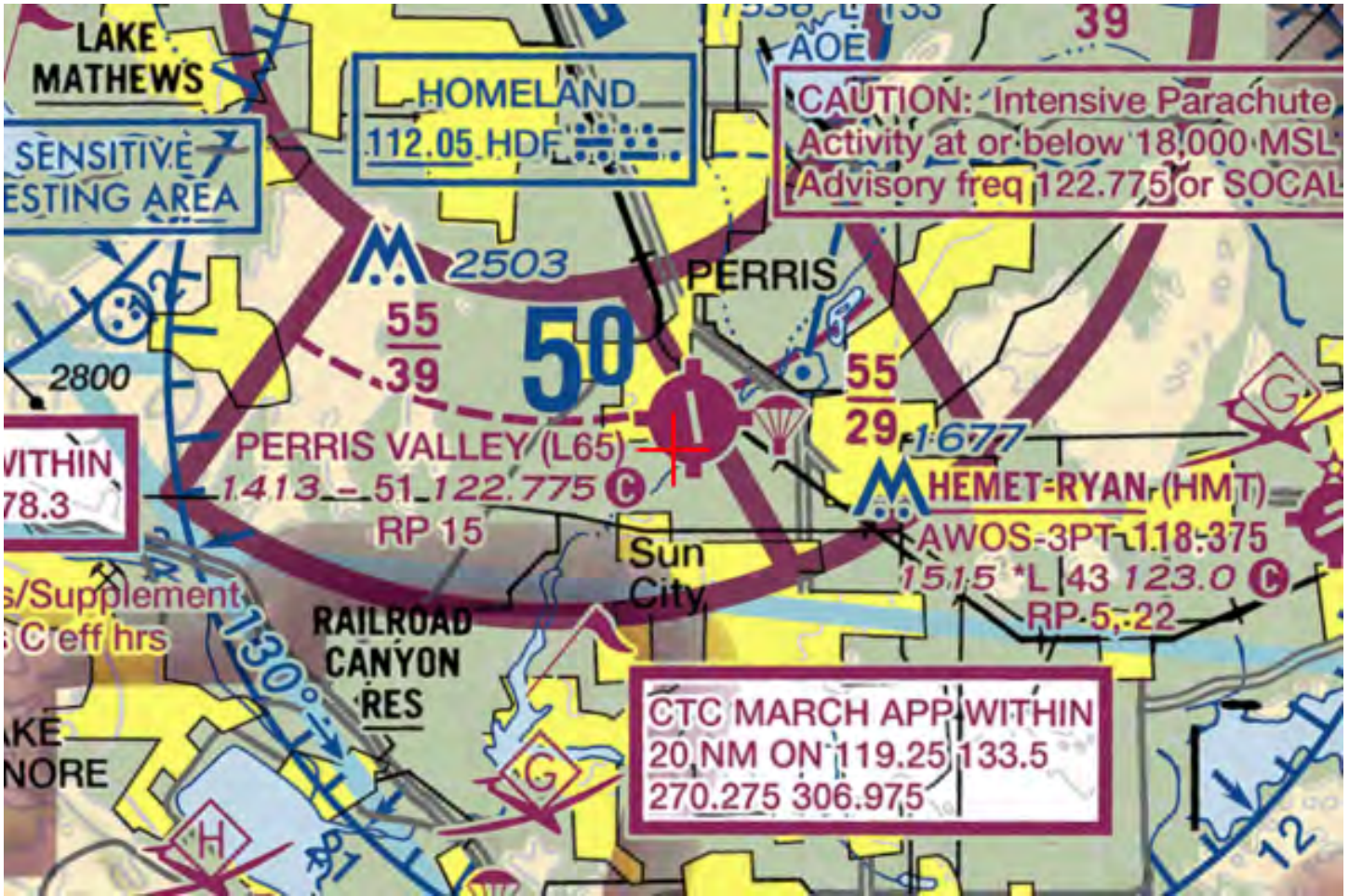
(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-4884-OE







Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2023-AWP-4885-OE

Issued Date: 03/13/2023

Brandon Dickens
IDI Logistics
840 Apollo Street, Suite 343
El Segundo, CA 90245

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Building Perris Logistic Center - Building 3
Location:	Perris, CA
Latitude:	33-45-24.64N NAD 83
Longitude:	117-13-34.25W
Heights:	1417 feet site elevation (SE) 52 feet above ground level (AGL) 1469 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 09/13/2024 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-4885-OE.

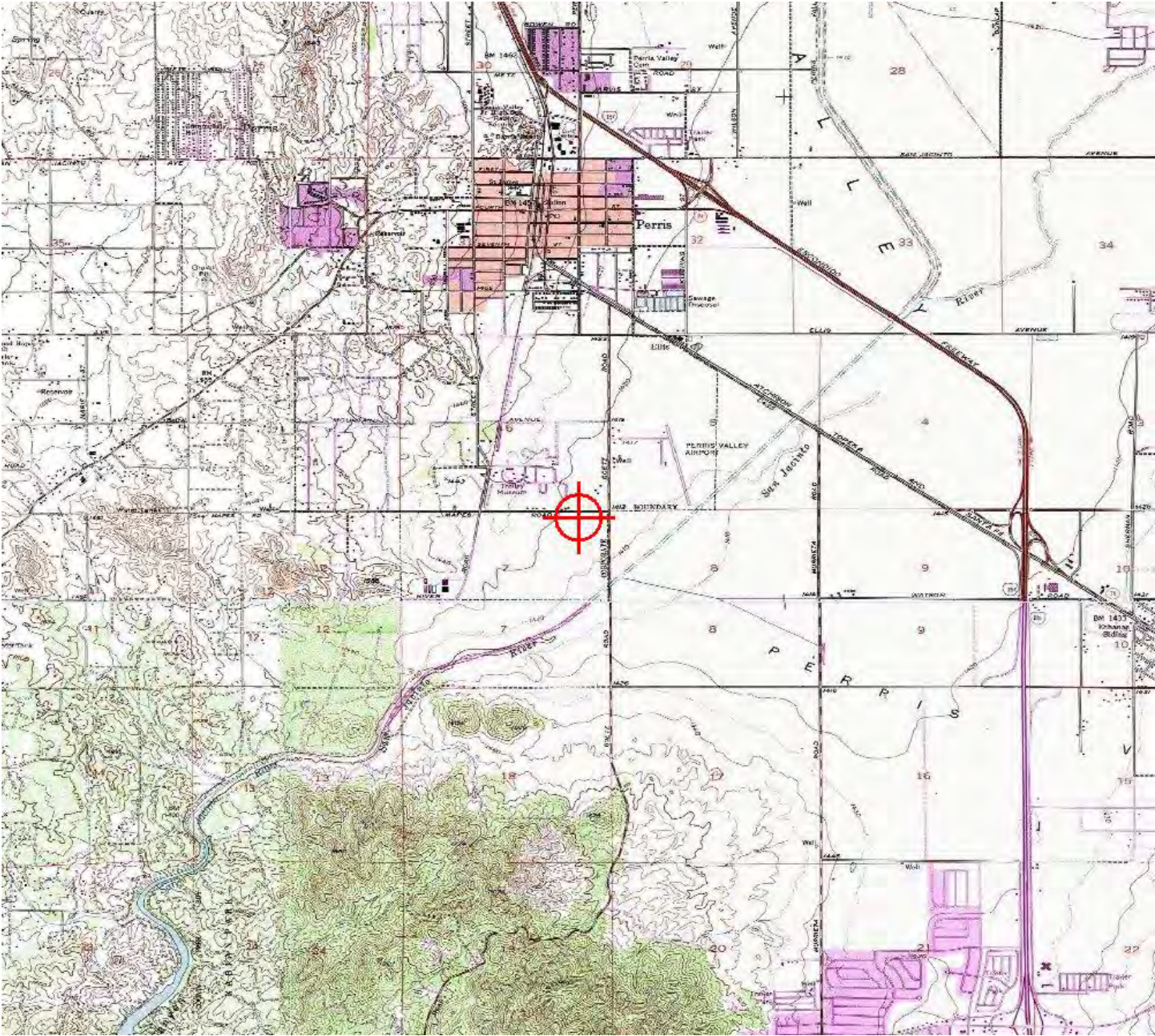
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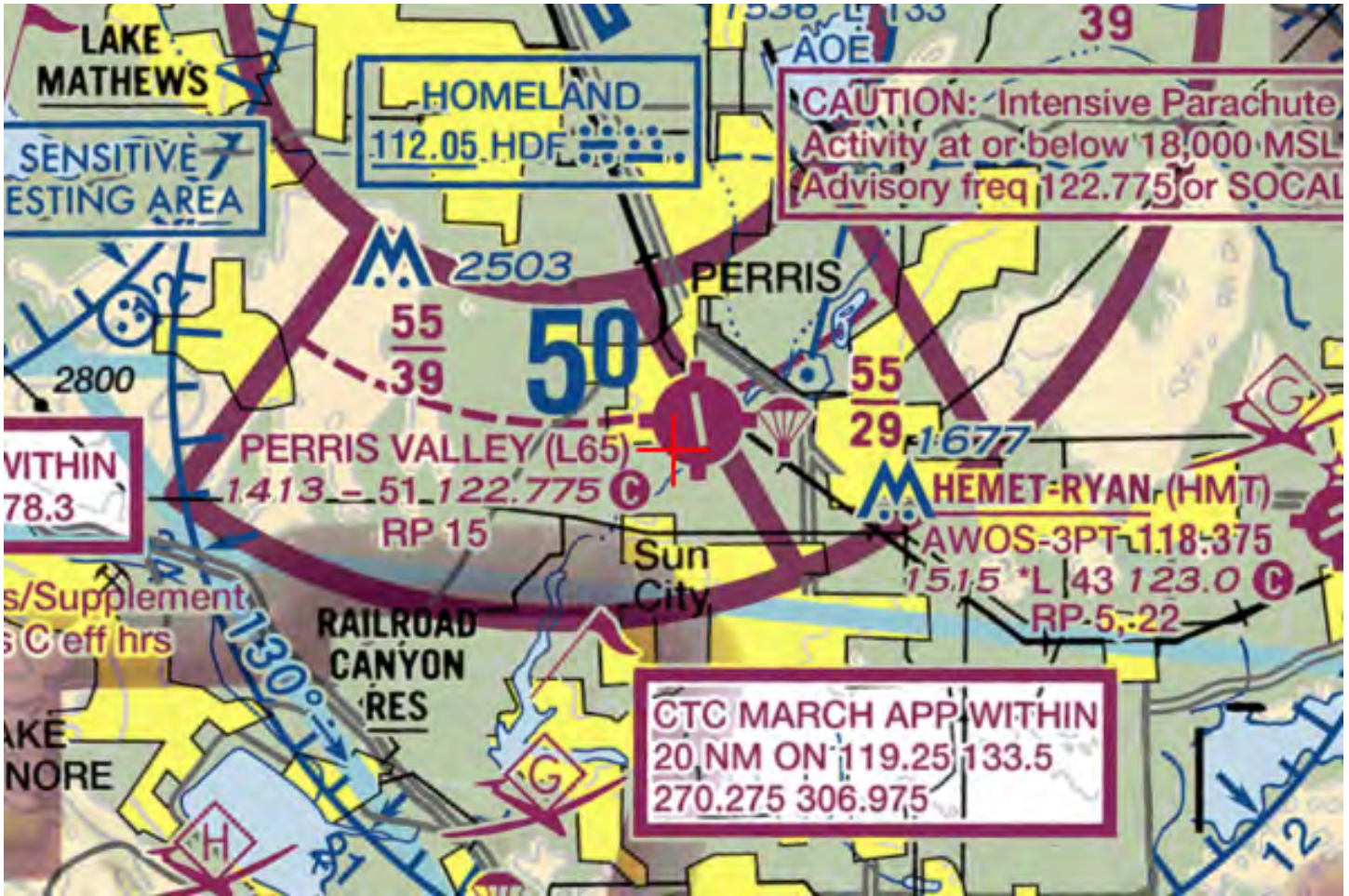
(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-4885-OE







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-4886-OE

Issued Date: 03/13/2023

Brandon Dickens
 IDI Logistics
 840 Apollo Street, Suite 343
 El Segundo, CA 90245

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building Perris Logistic Center - Building 3
 Location: Perris, CA
 Latitude: 33-45-12.98N NAD 83
 Longitude: 117-13-31.07W
 Heights: 1414 feet site elevation (SE)
 52 feet above ground level (AGL)
 1466 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 09/13/2024 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-4886-OE.

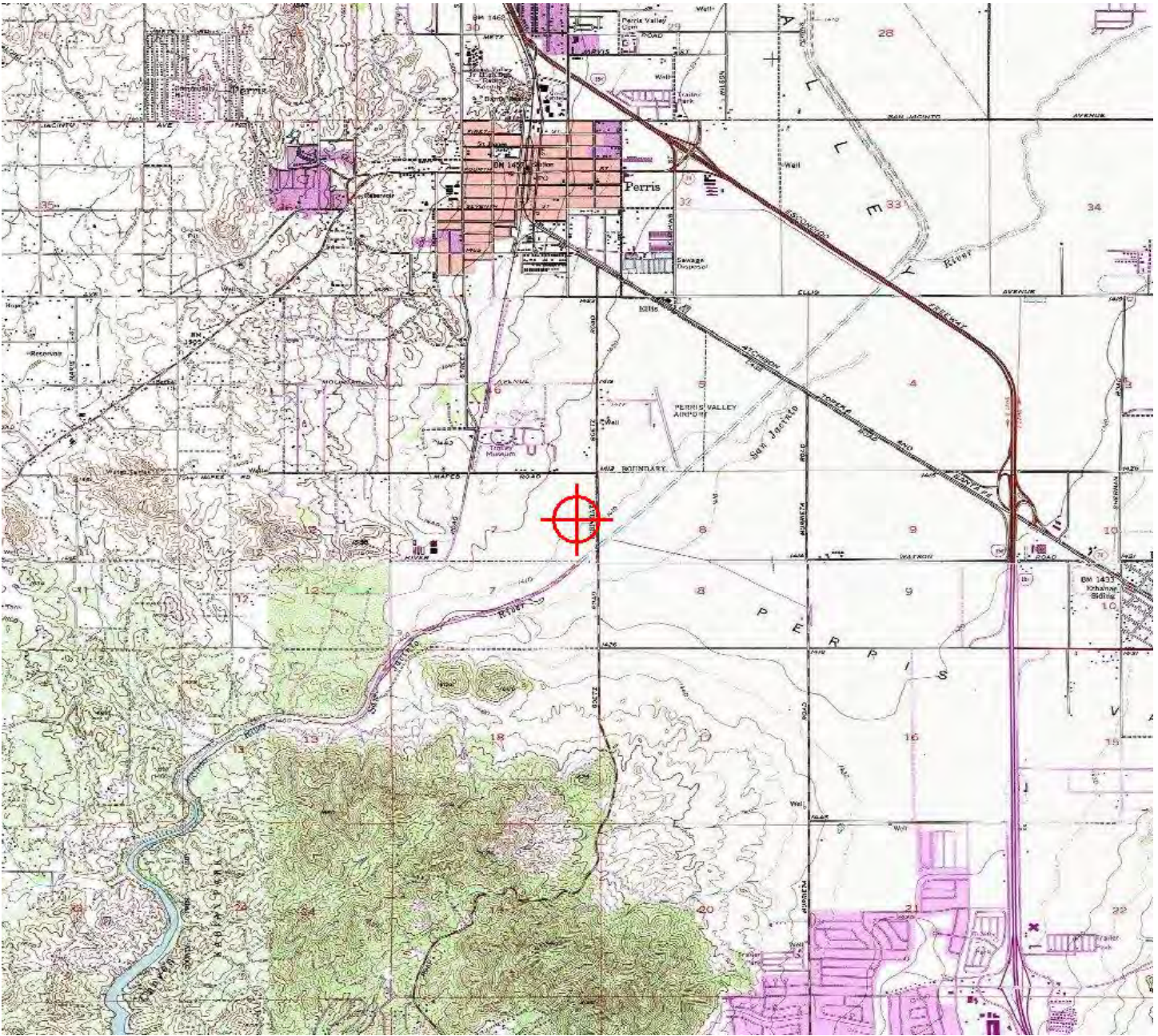
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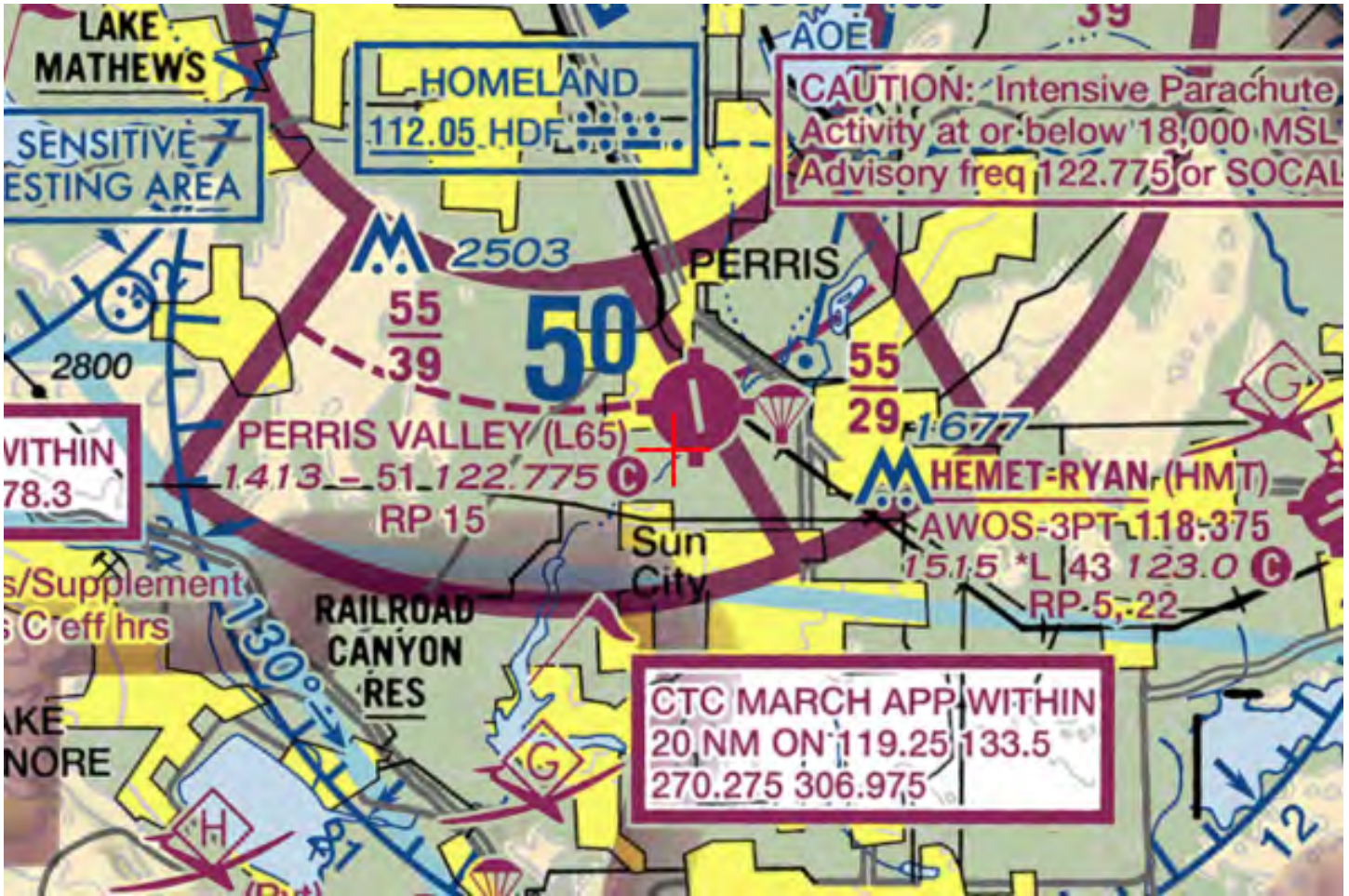
(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-4886-OE







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-4887-OE

Issued Date: 03/13/2023

Brandon Dickens
 IDI Logistics
 840 Apollo Street, Suite 343
 El Segundo, CA 90245

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building Perris Logistic Center - Building 3
 Location: Perris, CA
 Latitude: 33-45-13.23N NAD 83
 Longitude: 117-13-36.44W
 Heights: 1416 feet site elevation (SE)
 52 feet above ground level (AGL)
 1468 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 09/13/2024 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-4887-OE.

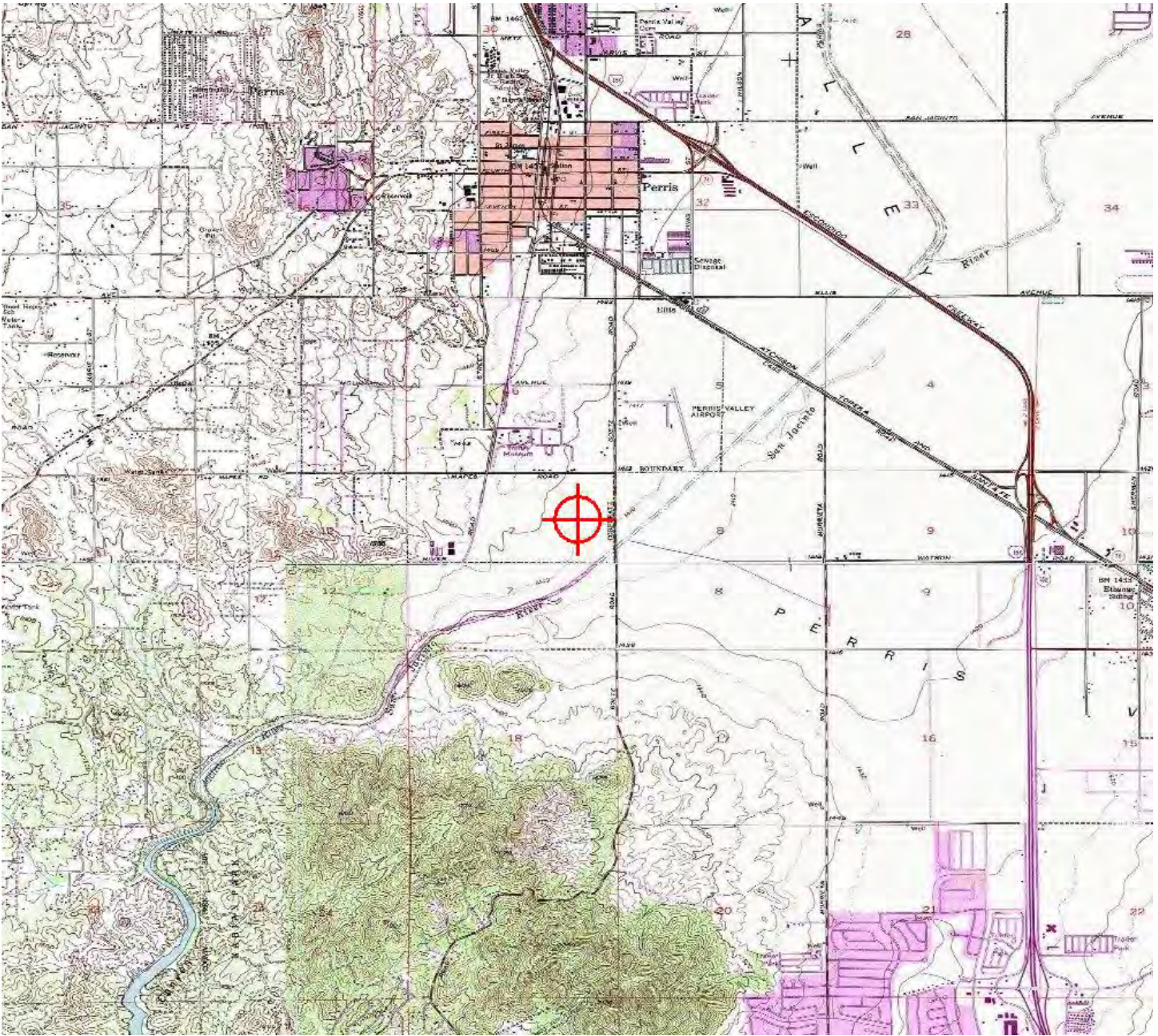
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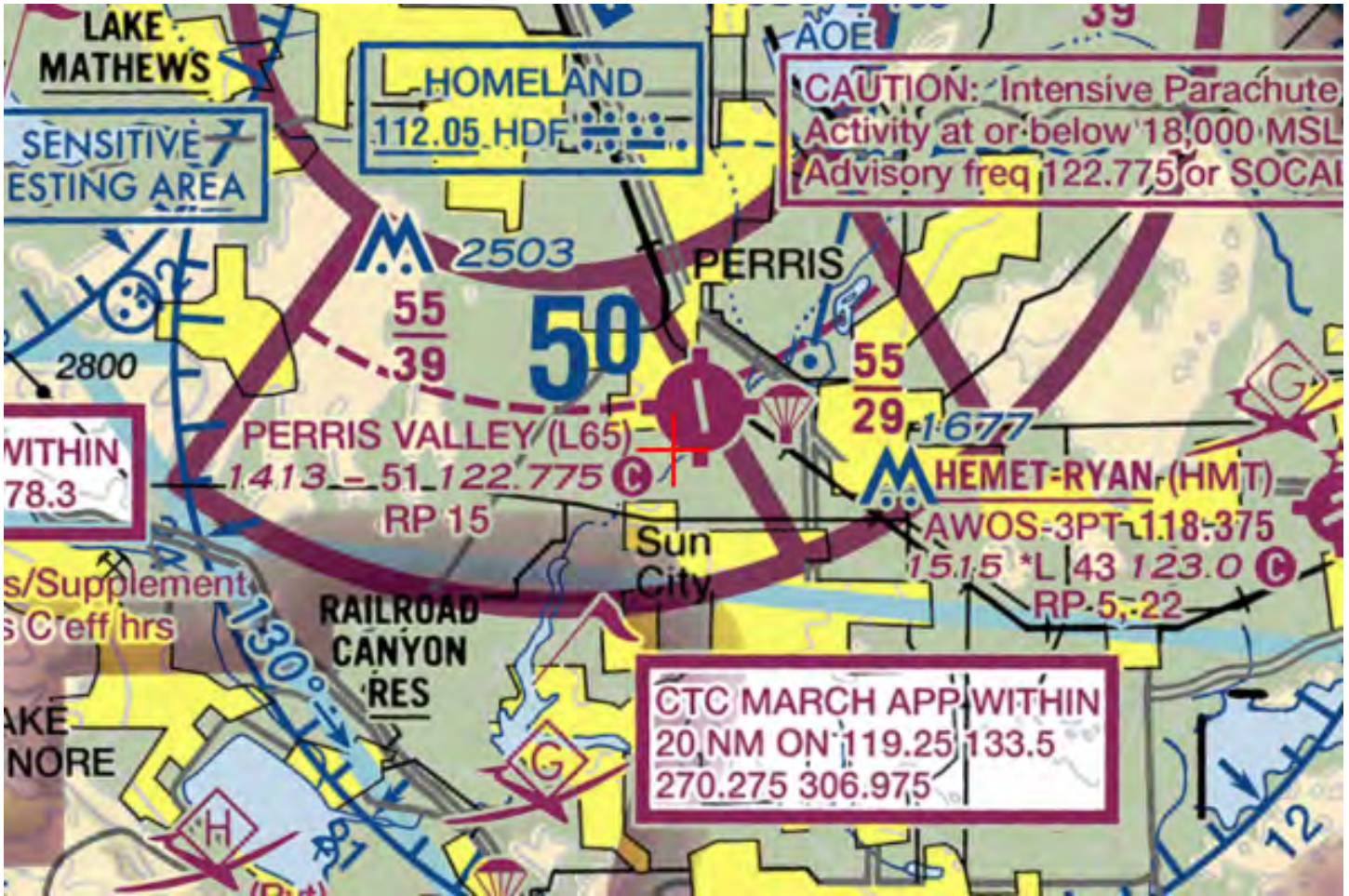
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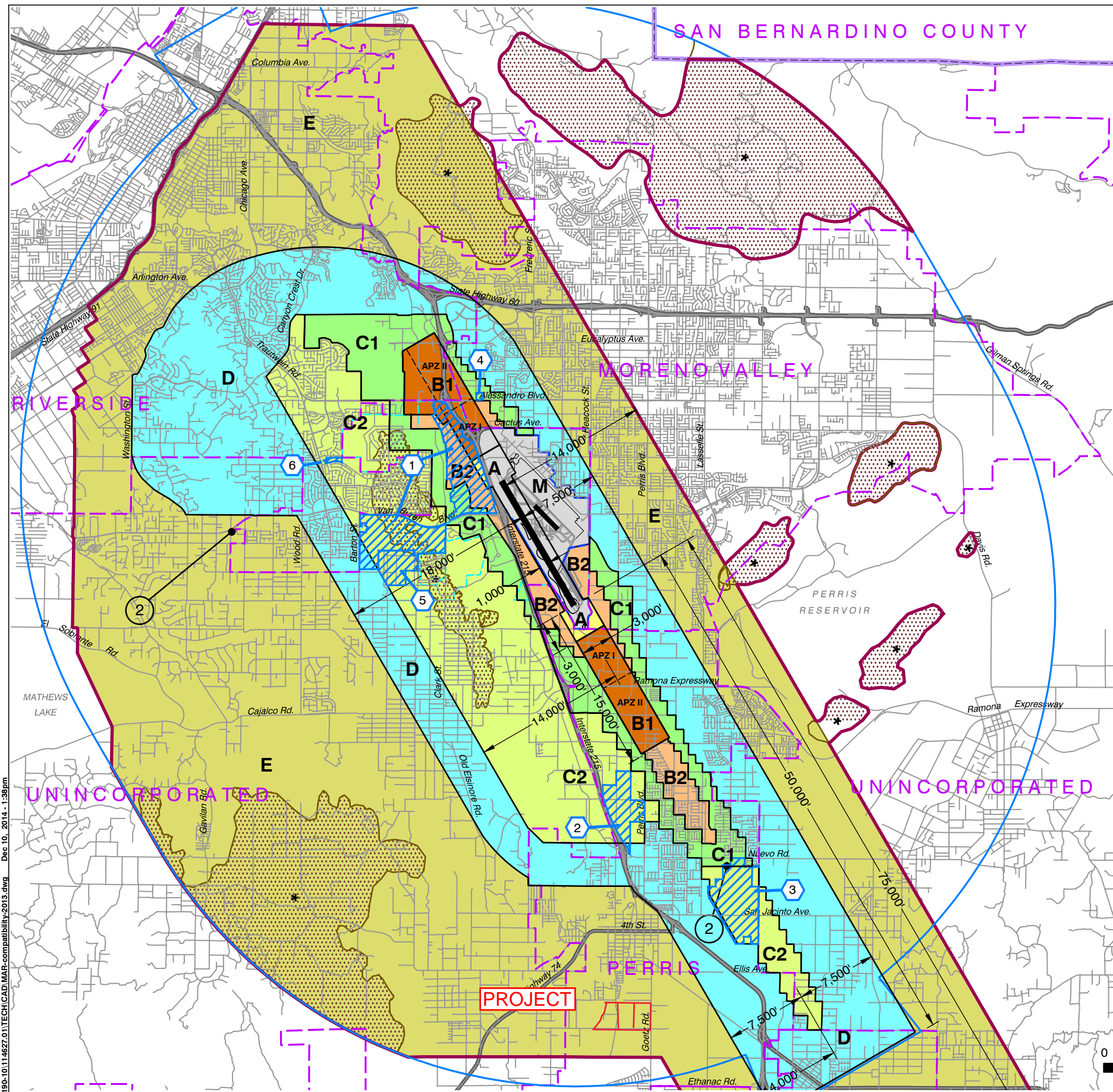
Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-4887-OE







LEGEND

Compatibility Zones

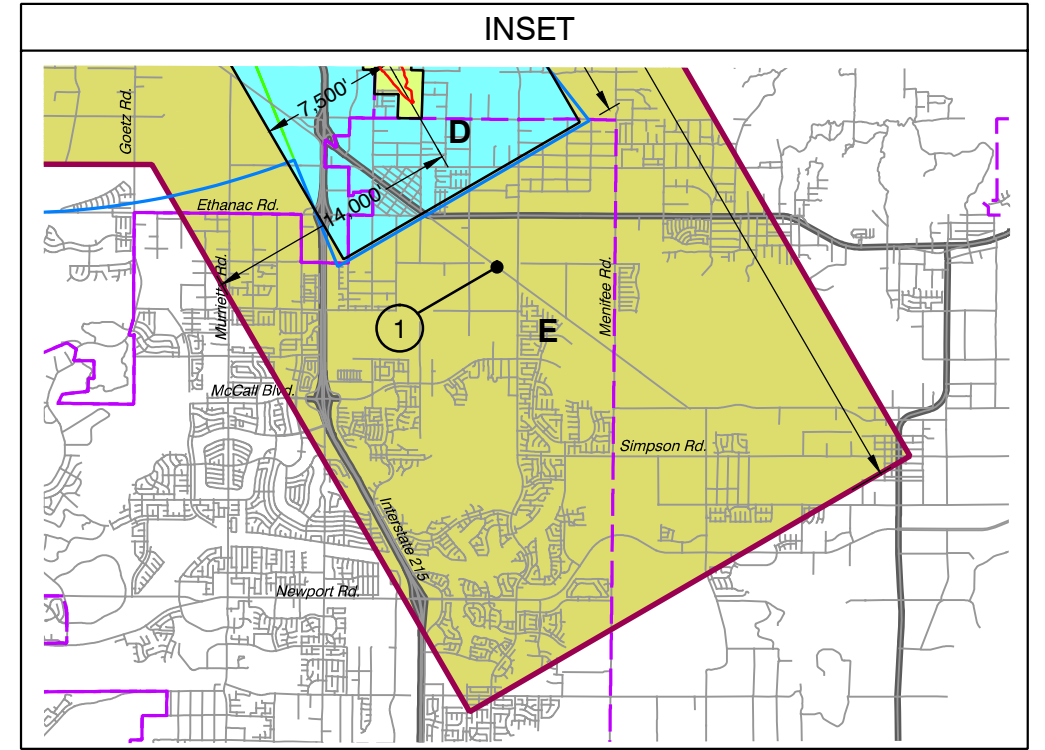
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

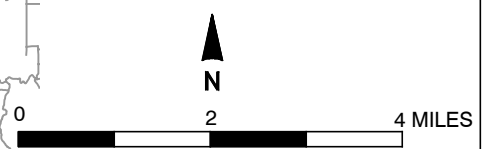
- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



**Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)**

Note:
All dimensions are measured from runway ends and centerlines.



Base map source: County of Riverside 2013

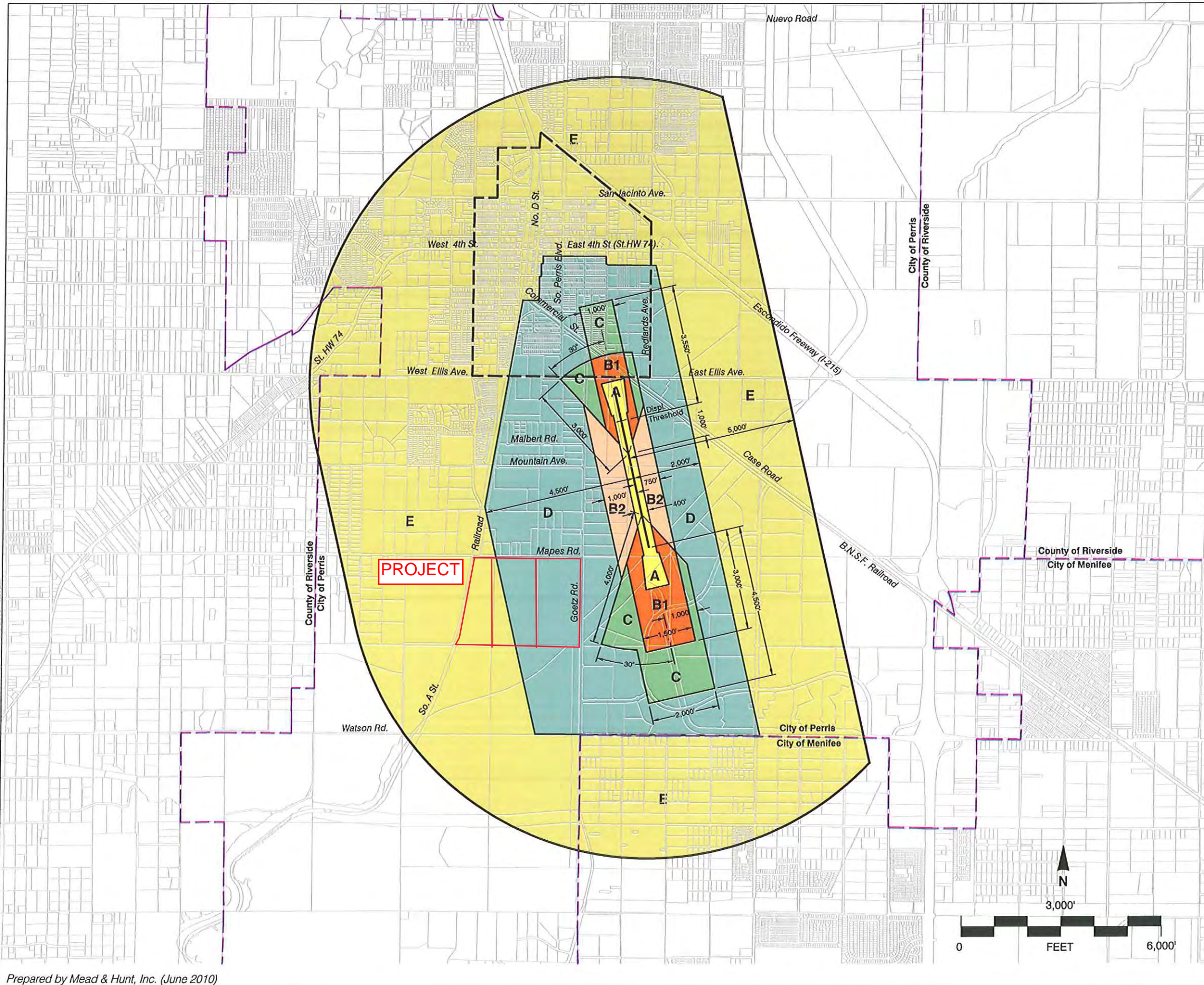
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Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map MA-1

**Compatibility Map
March Air Reserve Base / Inland Port Airport**



Legend

Compatibility Zones

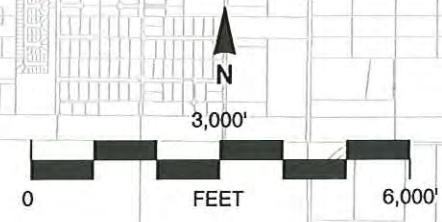
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E

Boundary Lines

- Airport Property Line
- City Limits
- Downtown Specific Plan

PROJECT

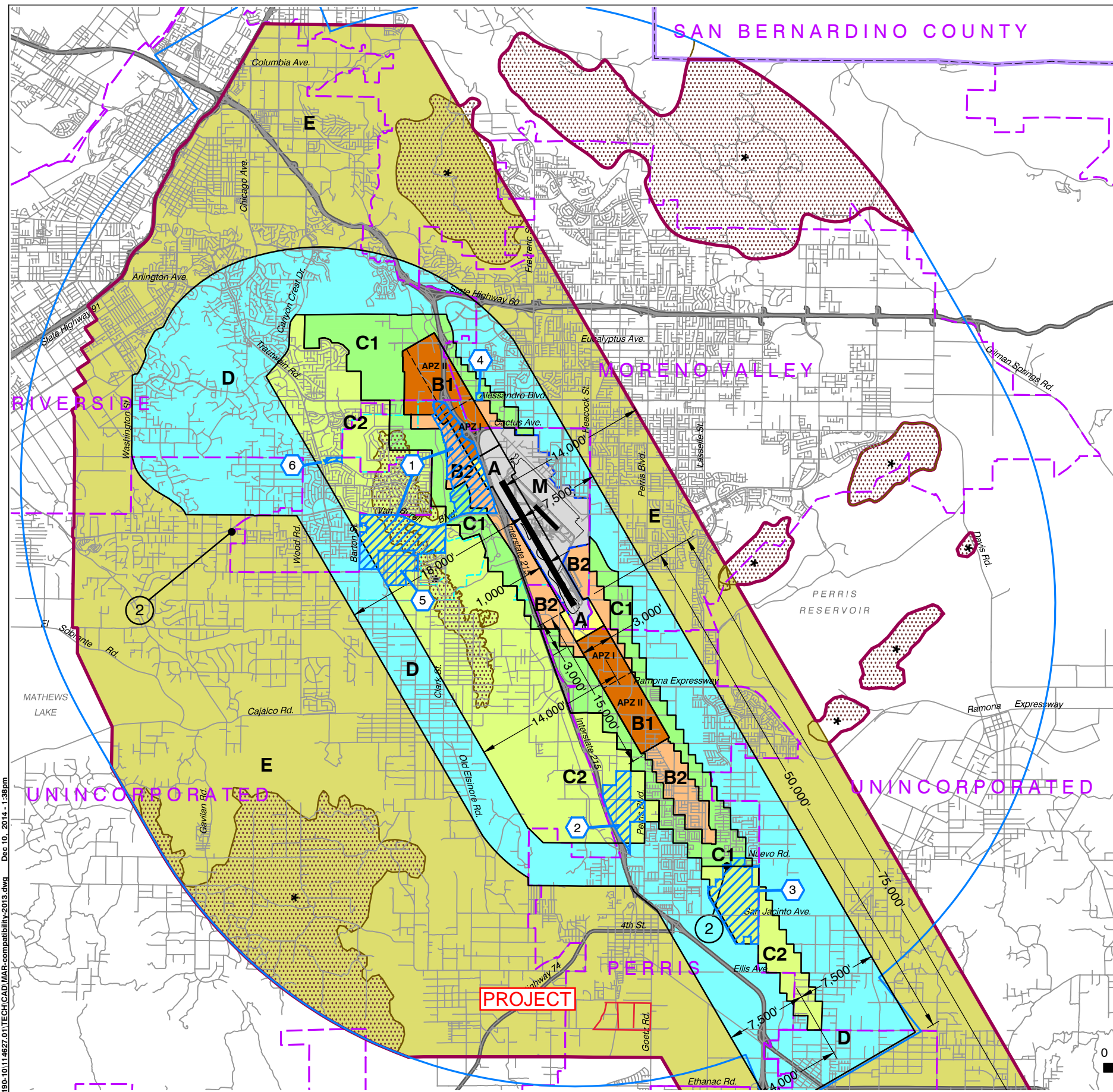
Riverside County
 Airport Land Use Commission
 Riverside County
 Airport Land Use Compatibility Plan
 Policy Document
 (July 2010 Draft)



Map PV-1

Compatibility Map
 Perris Valley Airport

C:\Users\j27\mead\perris\perris_valley\compatibility\2010.dwg Jul 01, 2010, 1:57pm
 Prepared by Mead & Hunt, Inc. (June 2010)



LEGEND

Compatibility Zones

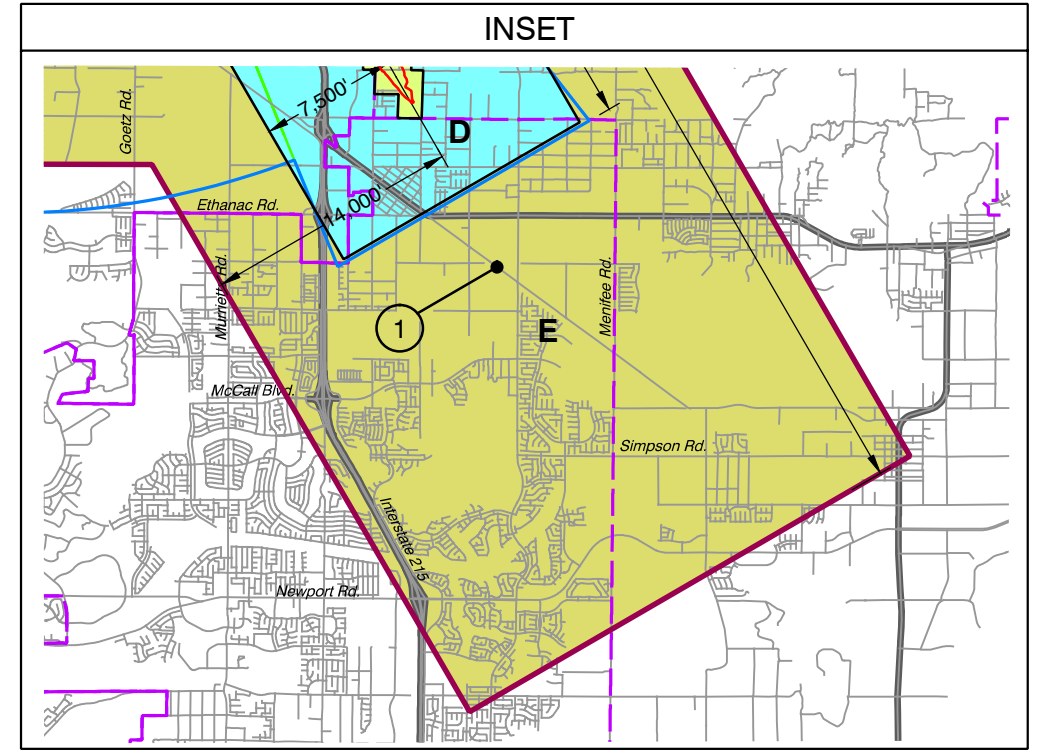
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

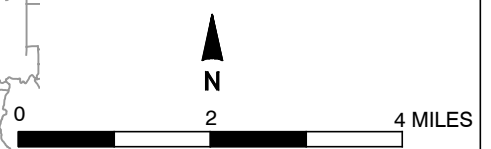
- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



**Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)**

Note:
All dimensions are measured from runway ends and centerlines.



Base map source: County of Riverside 2013

X:\18190-10\114627\01\TECH\CAD\MAR-compatibility\2013.dwg Dec 10, 2014 - 1:38pm

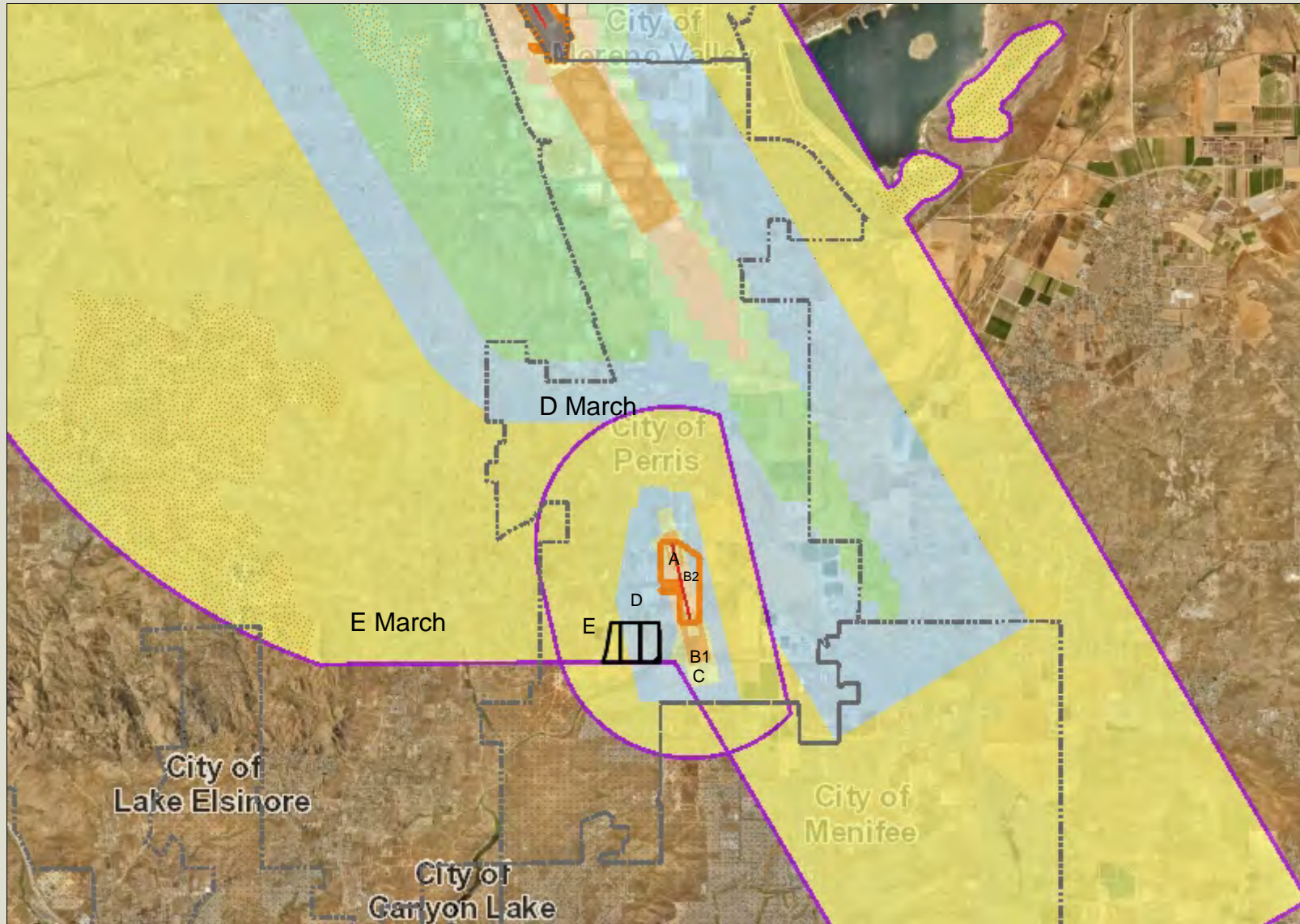
Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map MA-1

**Compatibility Map
March Air Reserve Base / Inland Port Airport**

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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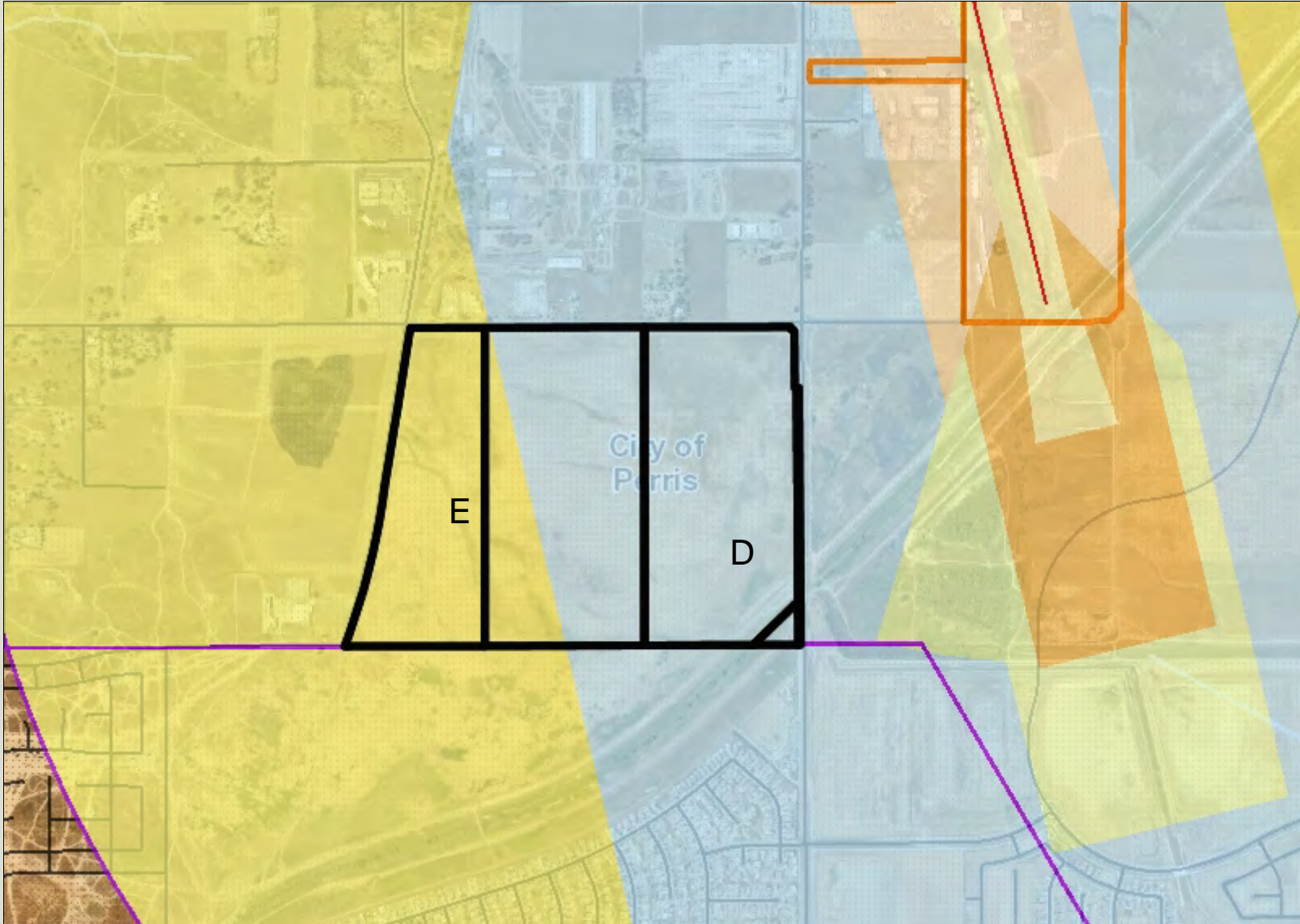


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Notes

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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Notes

Map My County Map



Legend

- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



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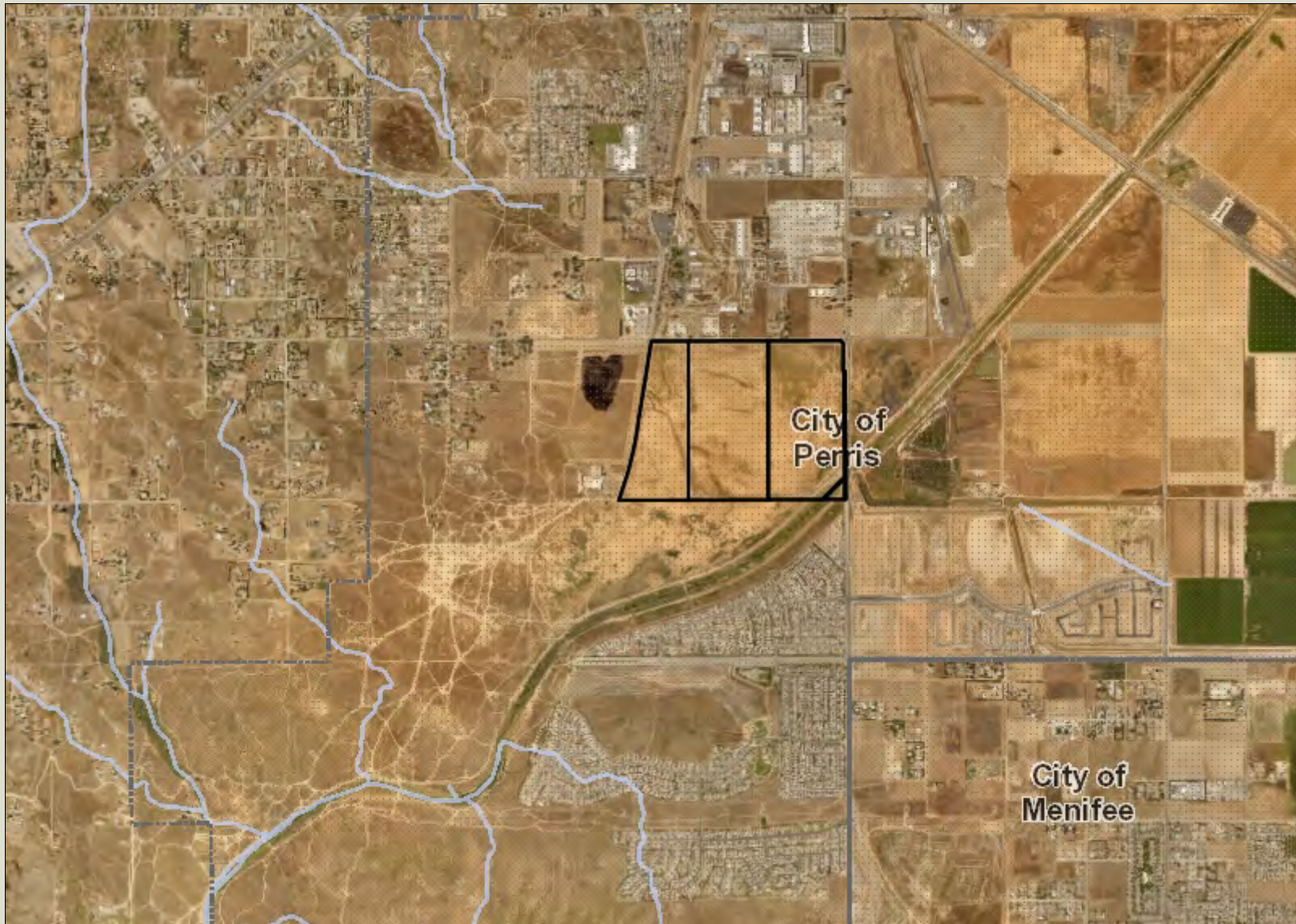
0 770 1,539 Feet

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Notes

Map My County Map



Legend

- Blue Line Streams
- City Areas
- World Street Map

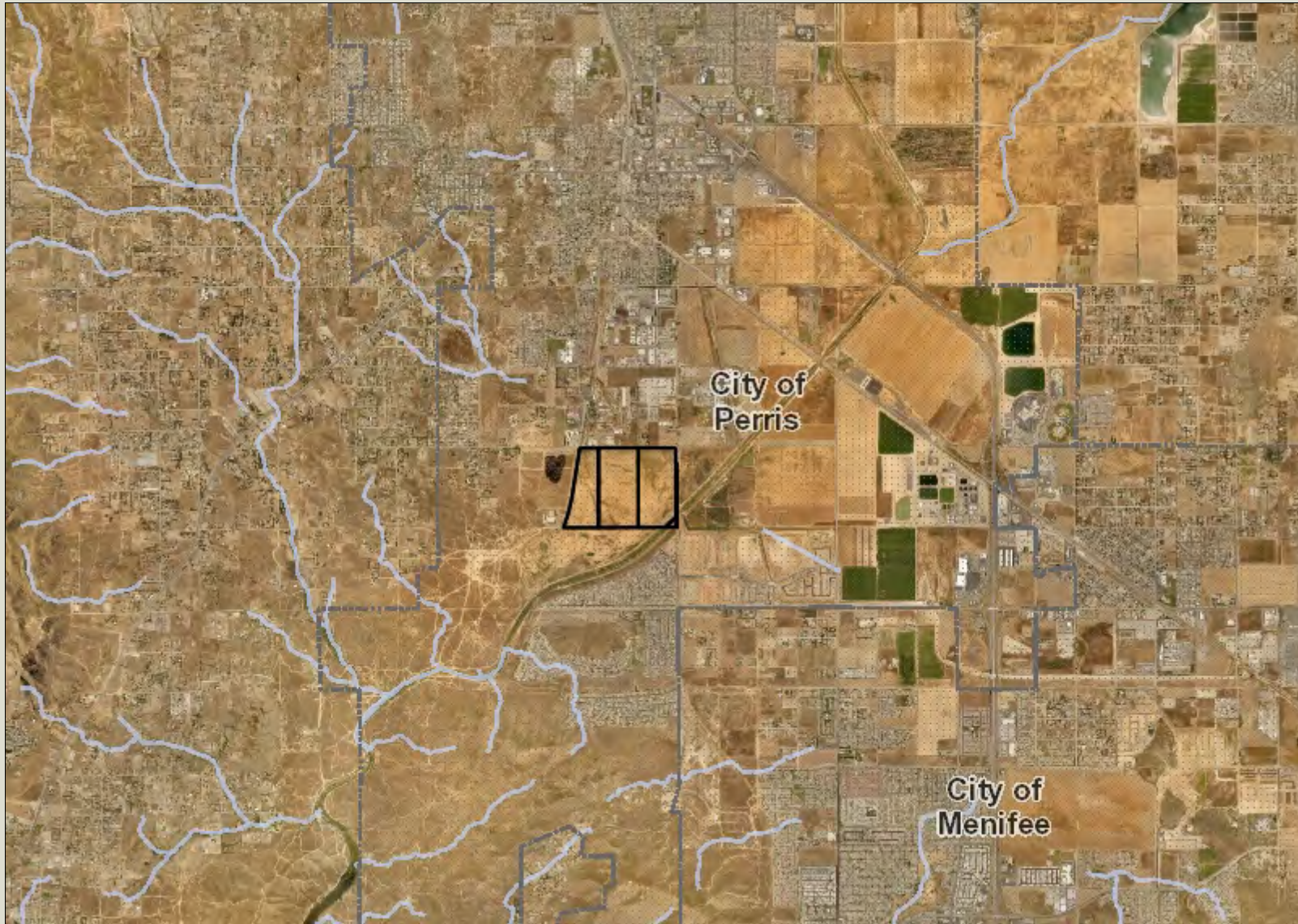


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Notes



Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map

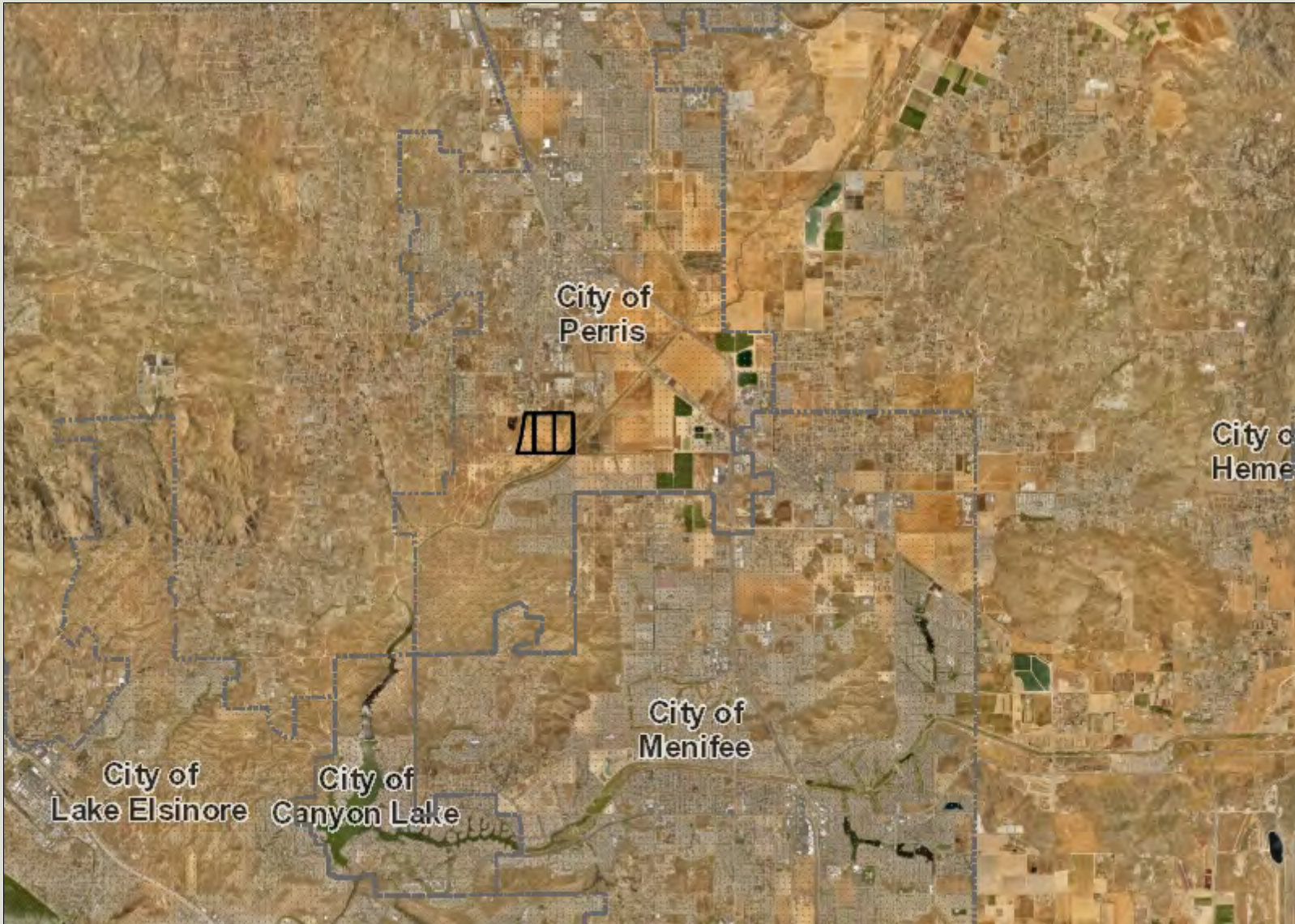


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Notes



Map My County Map



Legend

- City Areas
- World Street Map



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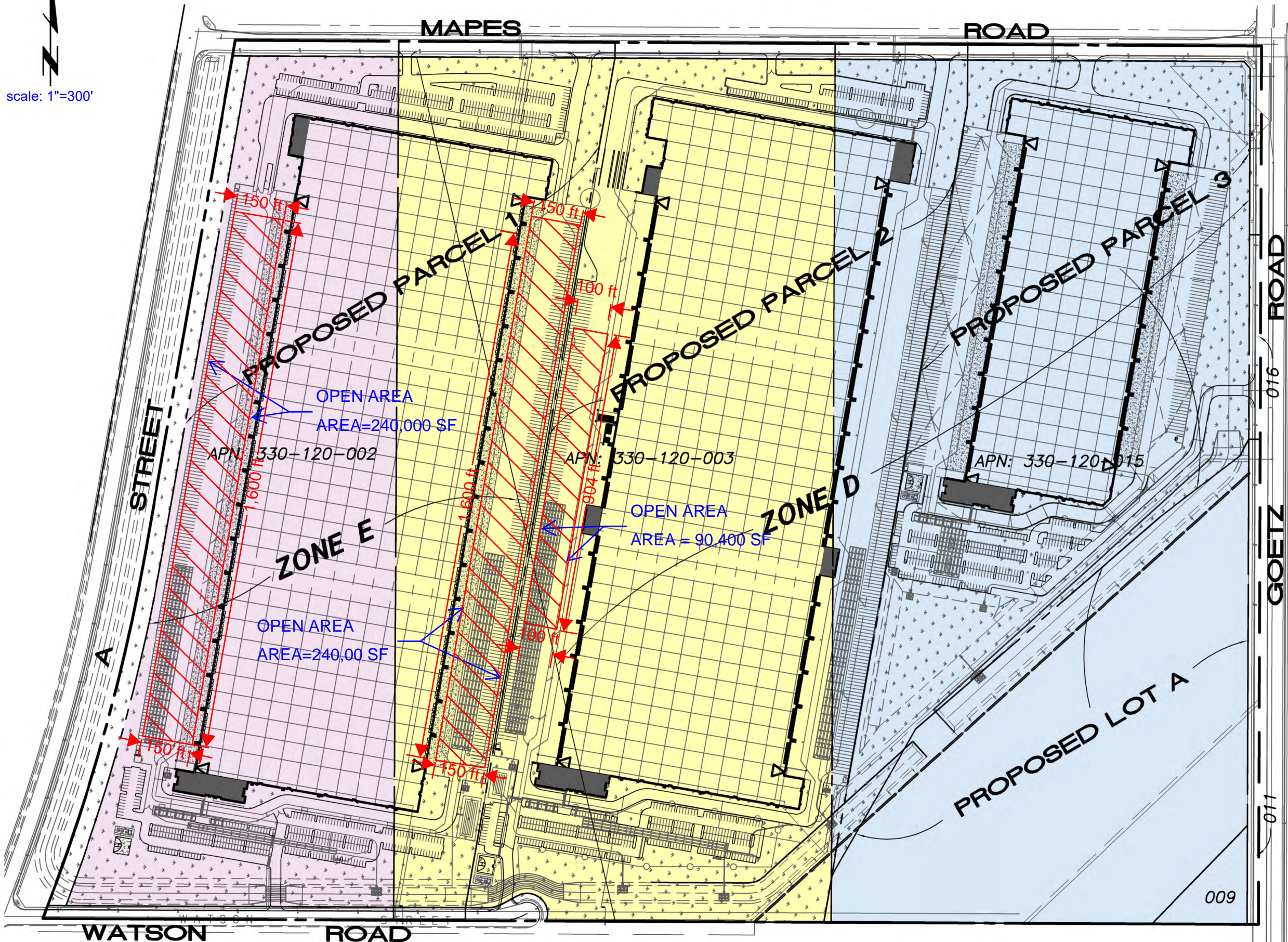
REPORT PRINTED ON... 3/28/2023 10:53:02 AM

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Notes

EXHIBIT
AREA EXHIBIT

scale: 1"=300'





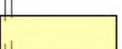

PARCEL AREA BREAKDOWN PER PARCEL MAP:

PARCEL 1: 65.174 AC.
PARCEL 2: 64.098 AC.
PARCEL 3: 37.108 AC.
LOT A: 25.391 AC.

AIRPORT ZONE AREA BREAKDOWN:

ZONE E:
PARCEL 1: 55.125 AC.
PARCEL 2: 5.570 AC.
TOTAL ZONE E: 60.695 AC.

ZONE D:
PARCEL 1: 10.049 AC.
PARCEL 2: 58.528 AC.
PARCEL 3: 37.108 AC.
LOT A: 25.391 AC.
TOTAL ZONE D: 131.076 AC.

-  = OPEN AREA (570,400 SF)
-  = APN: 330-120-002
-  = APN: 330-120-003
-  = APN: 330-120-015
- APN: 330-120-016
- APN: 330-120-009
- APN: 330-120-011

009

TENTATIVE PARCEL MAP NO. 38518

IN THE CITY OF PERRIS, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA,

BEING A SUBDIVISION OF LOTS 1, 2, 3, 8, 9 AND 10 OF JOHNSON'S SUBDIVISION, AS PER MAP FILED IN BOOK 15, PAGE 705 OF MAPS, SAN DIEGO COUNTY RECORDS, TOGETHER WITH THAT CERTAIN UNNAMED STREET LYING SOUTH OF LOTS 1, 2 AND 3, AS ABANDONED BY RESOLUTION RECORDED JULY 19, 1960, AS INSTRUMENT NO. 64051 OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA.

APNS:

330-120-002
330-120-003
330-120-009
330-120-011
330-120-015
330-120-016

NOTES:

DATE OF PREPARATION: 7-13-2022
GROSS ACREAGE: 201.643
NET ACREAGE: 191.771
NUMBER OF PROPOSED PARCELS: 3
LETTERED LOTS: A

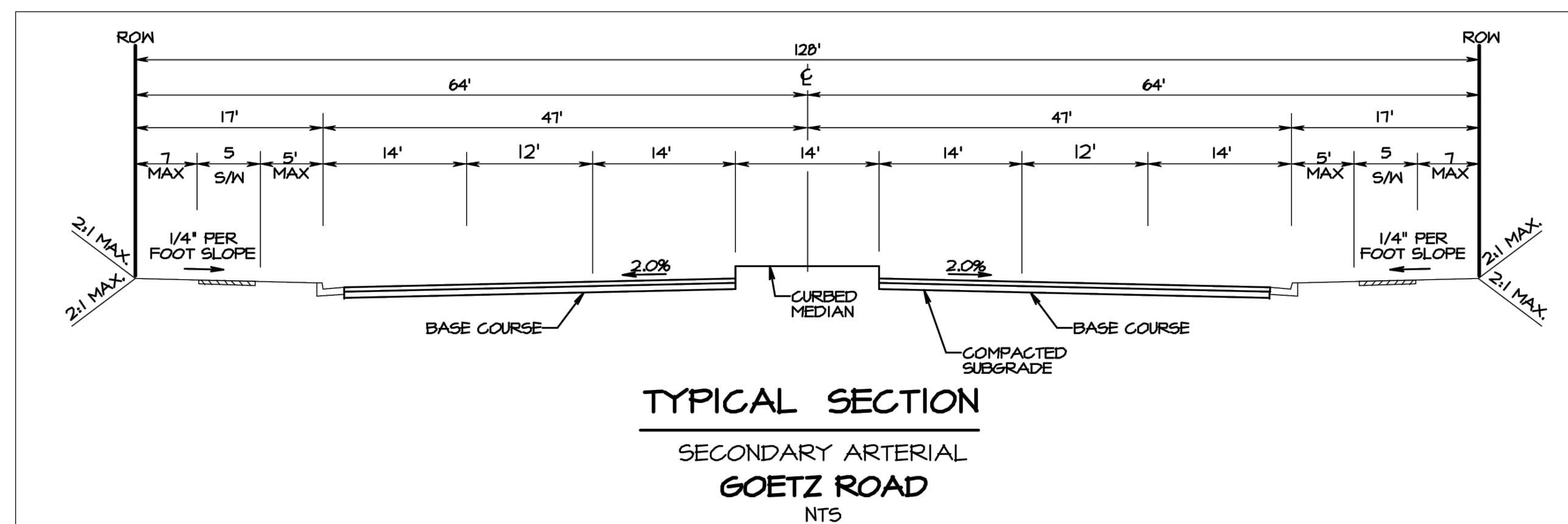
LEGAL DESCRIPTION:

REAL PROPERTY IN THE CITY OF PERRIS, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, DESCRIBED AS FOLLOWS:

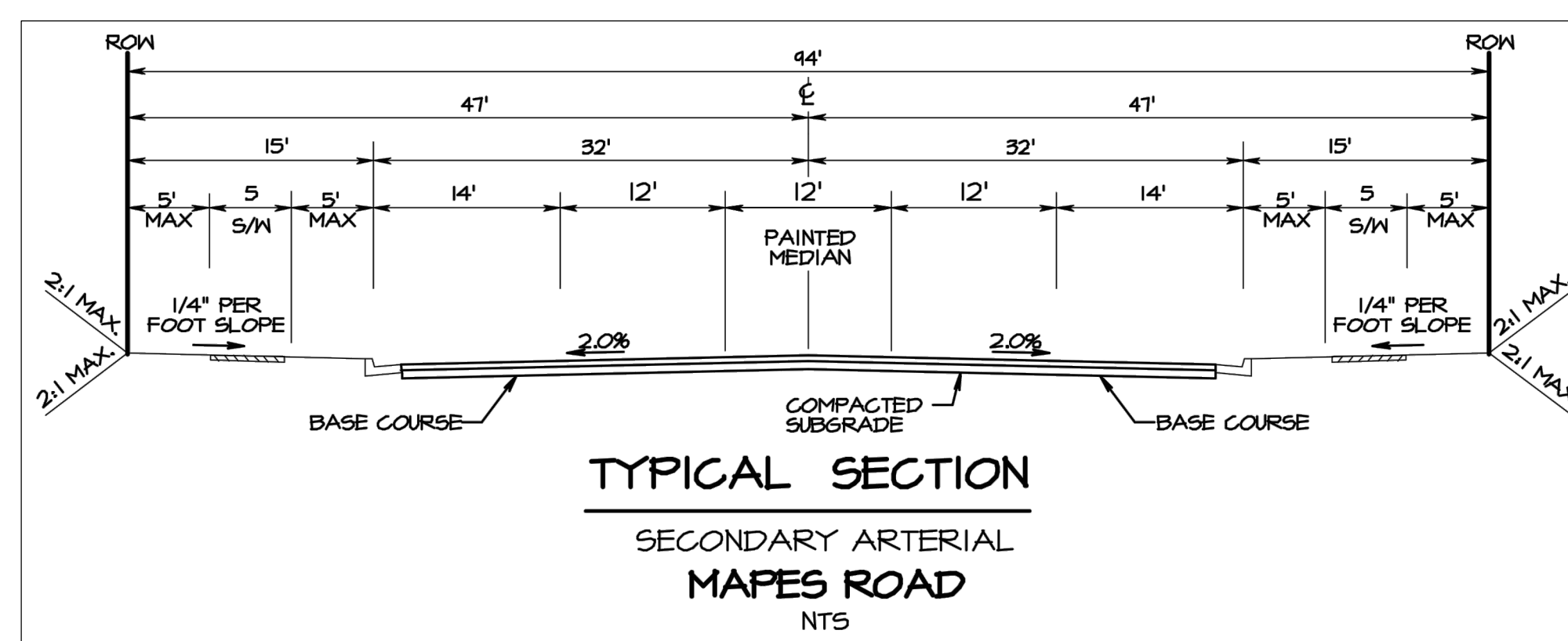
LOTS 1, 2, 3, 8, 9 AND 10 OF JOHNSON'S SUBDIVISION, AS SHOWN BY MAP ON FILE IN BOOK 15, PAGE 705 OF MAPS, RECORDS OF SAN DIEGO COUNTY, CALIFORNIA, TOGETHER WITH THAT CERTAIN UNNAMED ROAD LYING SOUTH OF LOTS 1, 2 AND 3, AND NORTH OF LOTS 8, 9, AND 10, AS ABANDONED BY THE COUNTY OF RIVERSIDE BY RESOLUTION ABANDONING COUNTY HIGHWAY, A CERTIFIED COPY OF WHICH WAS RECORDED JULY 19, 1960, AS INSTRUMENT NO. 64051 OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA.

EXCEPTING THEREFROM THE EAST 30 FEET OF LOTS 1 AND 10 FOR ROAD PURPOSES.

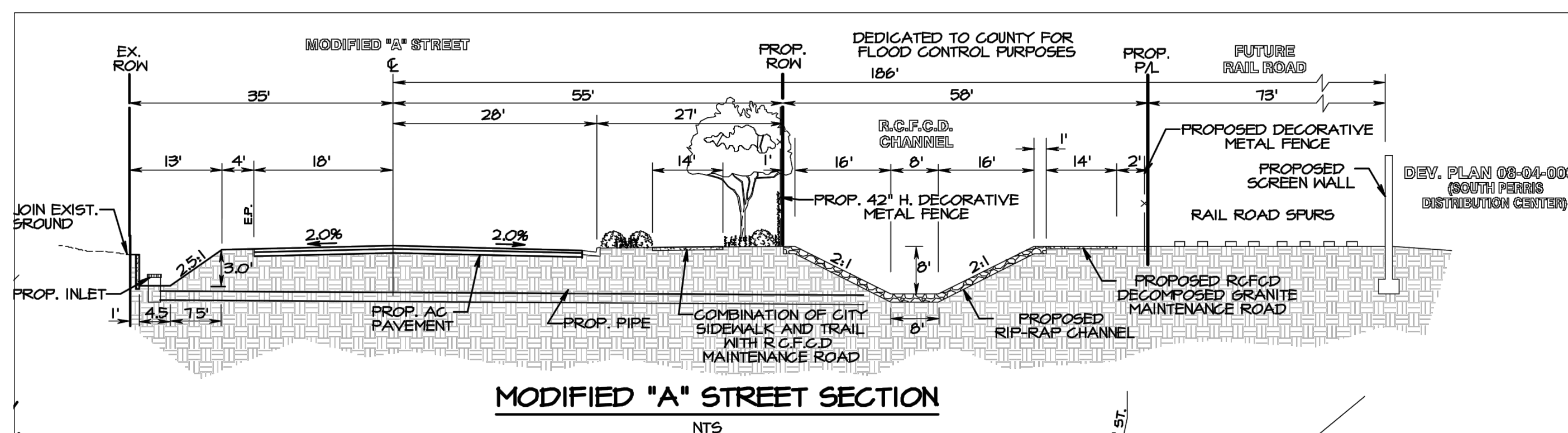
STREET CROSS SECTIONS:



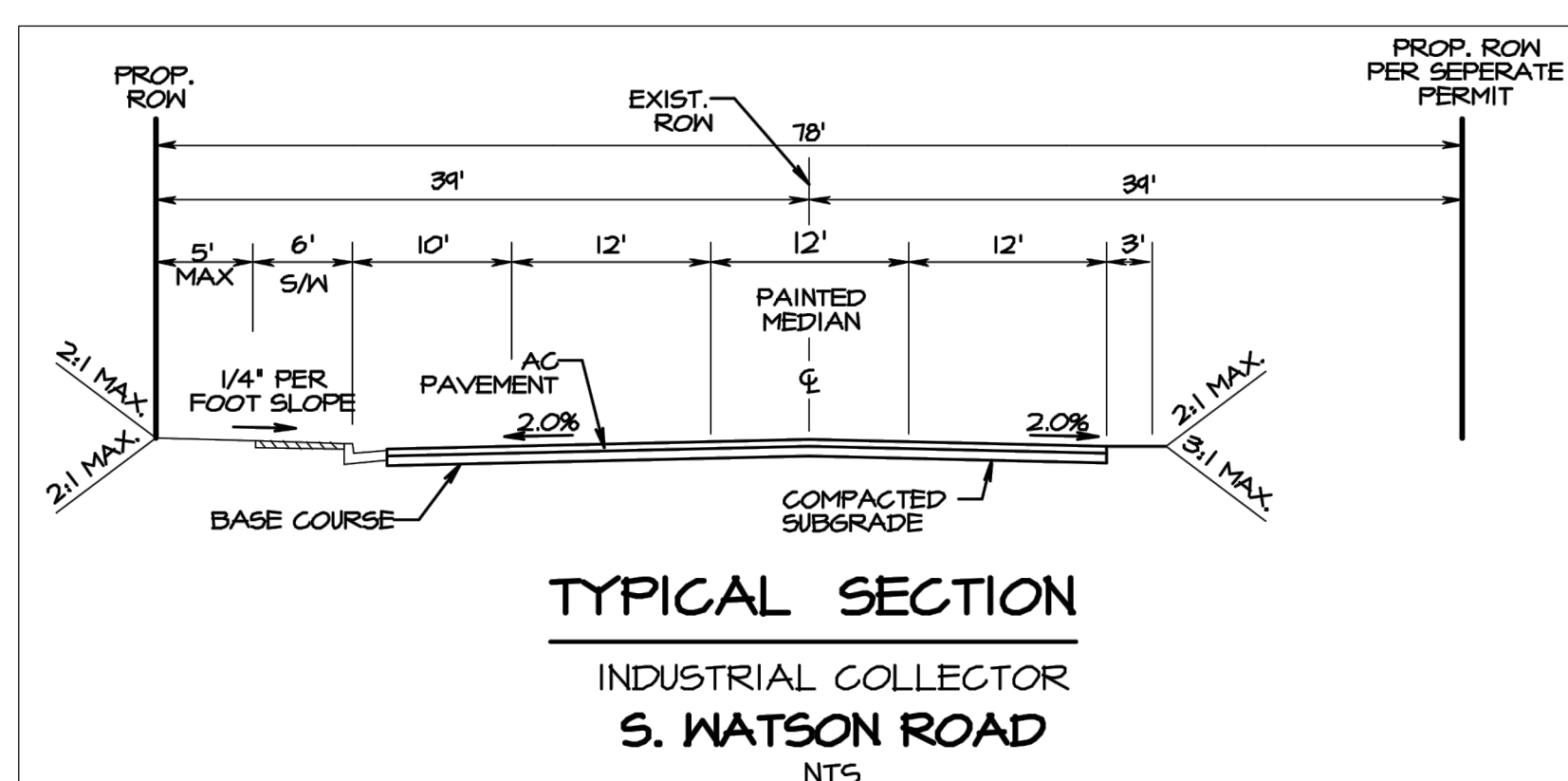
TYPICAL SECTION
SECONDARY ARTERIAL
GOETZ ROAD
NTS



TYPICAL SECTION
SECONDARY ARTERIAL
MAPES ROAD
NTS



MODIFIED "A" STREET SECTION
NTS



TYPICAL SECTION
INDUSTRIAL COLLECTOR
S. WATSON ROAD
NTS

BUILDING TABULATION:

	BLDG. 1	BLDG. 2	BLDG. 3	SUB-TOTAL	OPENSACE	TOTAL
SITE AREA						
In s.f.	2,983,782	2,796,047	1,616,265	7,396,094	1,265,932	8,662,026 s.f.
In acres	68.50	64.19	37.10	168.79	29.06	198.85 ac
BUILDING AREA						
Office	20,000	20,000	10,000	50,000		50,000 s.f.
Warehouse	1,365,090	1,404,920	523,574	3,293,584		3,293,584 s.f.
TOTAL	1,385,090	1,424,920	533,574	3,343,584		3,343,584 s.f.
CLEAR HEIGHT	42'	42'	40'			
COVERAGE	46.4%	51.0%	33.0%	45.2%		38.6%
AUTO PARKING REQUIRED						
Office: (office area is less than 10%)						
Whse: 1st 20K @ 1/1,000 s.f.	20	20	20	60		60 stalls
2nd 20K @ 1/2,000 s.f.	10	10	10	30		30 stalls
above 40K @ 1/5,000 s.f.	270	277	99	646		646 stalls
TOTAL	300	307	129	736		736 stalls
AUTO PARKING PROVIDED						
Standard (9' x 19')	412	308	207	927		927 stalls
Standard Accessible Parking (9' x 19')	9	6	5	20		20 stalls
Van Accessible Parking (12' x 19')	2	2	2	6		6 stalls
EVCS (9' x 19')	27	20	14	61		61 stalls
EV Capable (9' x 19')	80	60	41	181		181 stalls
Standard EV ADA (9' x 19')	1	1	1	3		3 stalls
VAN EV (12' x 19')	1	1	1	3		3 stalls
Ambulatory (11' x 19')	1	0	0	1		1 stalls
TOTAL	533	398	271	1,202		1,202 stalls
TRAILER PARKING REQUIRED						
Trailer: 1/5,000 SF	278	285	107	670		670 stalls
TRAILER PARKING PROVIDED						
Trailer (10' x 55')	339	316	131	786		786 stalls
ZONING ORDINANCE FOR CITY						
Zoning Designation - General Industrial						
MAXIMUM BUILDING HEIGHT ALLOWED						
Height - 50'						
MAXIMUM FLOOR AREA RATIO						
FAR - 75						
LANDSCAPE REQUIREMENT						
Percentage - 10%						
SETBACKS						
Building						
Mapes St. - 10' (Local)						
Goetz St. - 15' (Arterial)						
A Street - 10' (Local)						
Side/Rear - none, adjoining R zone- 20'						

EXISTING EASEMENTS:

- 1. A RIGHT OF WAY GRANTED TO TEMESCAL WATER COMPANY, A CORPORATION, FOR PIPE AND DITCH LINE OVER A PORTION OF LOT 10 HEREIN DESCRIBED, AS PARTICULARLY SET OUT IN DEED RECORDED APRIL 6, 1901 IN BOOK 119 PAGE 347 OF DEEDS, RIVERSIDE COUNTY RECORDS. (SAID RIGHT OF WAY NOT PLOTTABLE FROM RECORD)
- 2. A RIGHT OF WAY OF THE SAN JACINTO RIVERSIDE FLOOD CONTROL CHANNEL, AS EVIDENCED BY RECORD OF SURVEY MAP BOOK 25, PAGE 4, RIVERSIDE COUNTY RECORDS. (CENTERLINE OF AN ABANDONED ROAD PLOTTED HEREON, EXACT WIDTH NOT DISCLOSED IN JOHNSON'S SUBDIVISION, AS SHOWN BY MAP ON FILE IN BOOK 15, PAGE 705 OF MAPS, RECORDS OF SAN DIEGO COUNTY, EASEMENT RESERVED FOR EXISTING AND FUTURE PUBLIC UTILITIES, PUBLIC SERVICE AND DRAINAGE FACILITIES AND STRUCTURES) (SAID EASEMENT TO BE ABANDONED ON THE PARCEL MAP)
- 3. THE RIGHTS, IF ANY, OF A CITY, PUBLIC UTILITY OR SPECIAL DISTRICT, PURSUANT TO SECTION 8348 ET SEQ. OF THE CALIFORNIA STREETS AND HIGHWAYS CODE, TO PRESERVE A PUBLIC EASEMENT IN UNNAMED ROAD AS THE SAME WAS VACATED BY THE DOCUMENT RECORDED JULY 19, 1960 AS INSTRUMENT NO. 64051 OF OFFICIAL RECORDS. (CENTERLINE OF AN ABANDONED ROAD PLOTTED HEREON, EXACT WIDTH NOT DISCLOSED IN JOHNSON'S SUBDIVISION, AS SHOWN BY MAP ON FILE IN BOOK 15, PAGE 705 OF MAPS, RECORDS OF SAN DIEGO COUNTY, EASEMENT RESERVED FOR EXISTING AND FUTURE PUBLIC UTILITIES, PUBLIC SERVICE AND DRAINAGE FACILITIES AND STRUCTURES) (SAID EASEMENT TO BE ABANDONED ON THE PARCEL MAP)
- 4. AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED SEPTEMBER 24, 1963 AS INSTRUMENT NO. 100697 OF OFFICIAL RECORDS, IN FAVOR OF: EASTERN MUNICIPAL WATER DISTRICT (EASEMENT PLOTTED HEREON)
- 5. AN OFFER OF DEDICATION FOR PUBLIC STREET AND HIGHWAY PURPOSES TOGETHER WITH ALL RIGHT TO CONSTRUCT AND MAINTAIN UTILITIES, SEWERS, DRAINS AND OTHER IMPROVEMENTS AND INCIDENTAL PURPOSES, RECORDED DECEMBER 08, 2020 AS INSTRUMENT NO. 2020-0626020 OF OFFICIAL RECORDS. TO: THE CITY OF PERRIS, A MUNICIPAL CORPORATION. A DOCUMENT ENTITLED "CERTIFICATE OF ACCEPTANCE-INDUSTRIAL DEVELOPERS REALTY, LLC-RESOLUTION NUMBER 5763" RECORDED JULY 06, 2021 AS INSTRUMENT NO. 2021-0405271 OF OFFICIAL RECORDS. (EASEMENT PLOTTED HEREON)

PROPOSED EASEMENTS:

- 1. AN EASEMENT FOR STREET AND PUBLIC UTILITY PURPOSES, TO BE DEDICATED AS AN EASEMENT TO THE CITY OF PERRIS ON THE PARCEL MAP.
- 2. AN EASEMENT FOR TRAIL PURPOSES, TO BE DEDICATED AS AN EASEMENT TO THE CITY OF PERRIS ON THE PARCEL MAP.

ZONING:

ZONING INFORMATION:
(THE FOLLOWING ZONE DESIGNATIONS ARE PER CITY OF PERRIS DEPARTMENT OF ENGINEERING / MAPPING)
GENERAL PLAN DESIGNATION: GI-GENERAL INDUSTRIAL
ZONE DESIGNATION: GI-GENERAL INDUSTRIAL
SPECIAL ZONE: MARB ZONE E - OTHER AIRPORT ENVIRONS

LINE TABLE

LINE #	BEARING	LENGTH
L1	N 61°28'26" E	50.00'

CURVE TABLE

CURVE #	DELTA	RADIUS	LENGTH
C1	28°47'56"	100.00'	50.26'
C2	139°56'21"	61.00'	148.99'

LEGEND:

Legend symbols and descriptions including CENTERLINE, CONCRETE, TREE, PALM TREE, IRON FENCE, BLOCK WALL, and various utility lines.



REVISIONS table with columns for NO., DESCRIPTION, and DATE.

APPLICANT:
IDI LOGISTICS
840 APOLLO STREET, SUITE 343
EL SEGUNDO, CA 90245
PHONE: (213) 330-8066

ARCHITECT:
HPA ARCHITECTURE
18831 BARDEEN AVENUE, SUITE 100
IRVINE, CA 92612
PHONE: (949) 863-1770

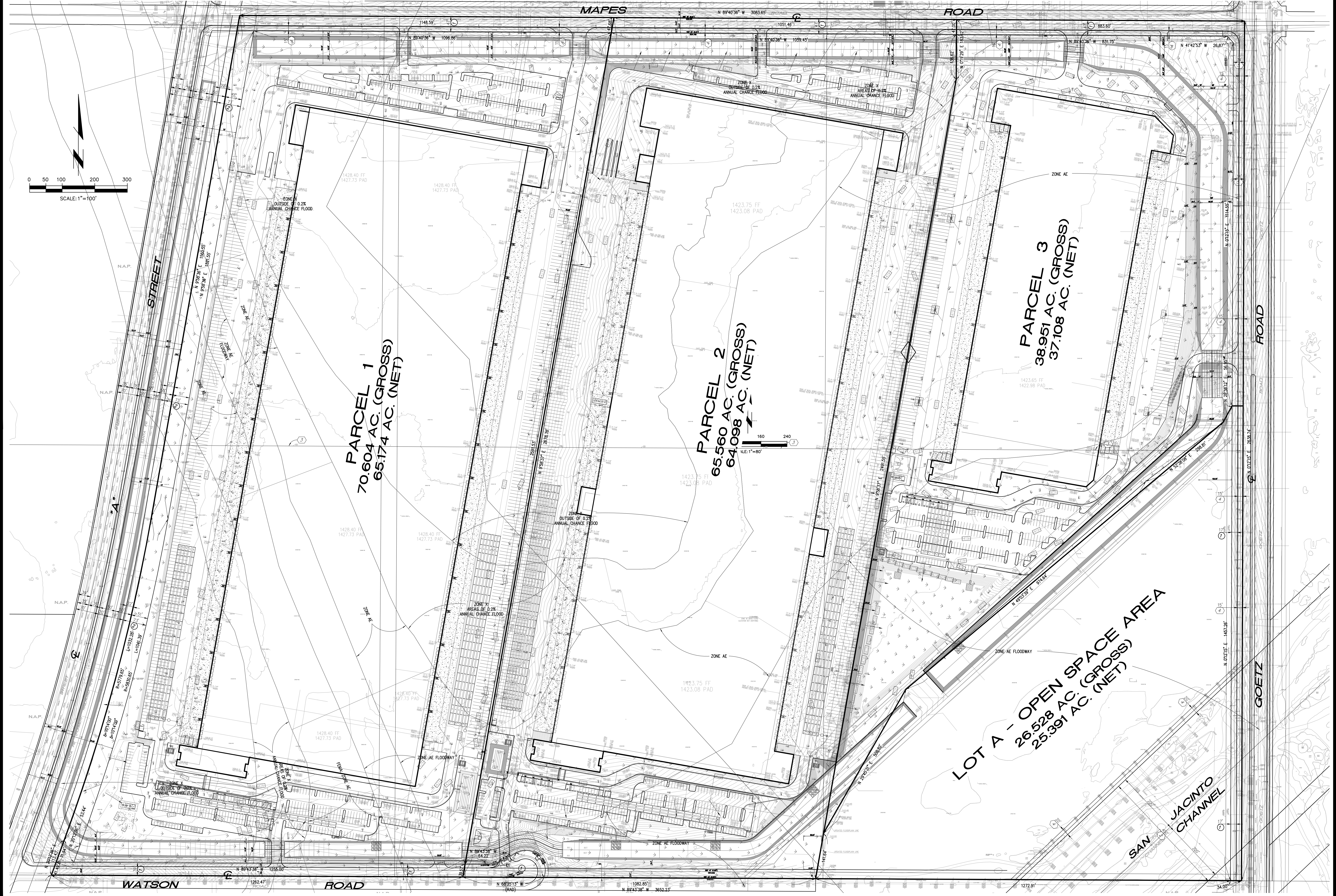
CIVIL ENGINEER:
Thienes Engineering, Inc.
CIVIL ENGINEERING • LAND SURVEYING
14349 FIRESTONE BOULEVARD
LA MIRADA, CALIFORNIA 90638
PH (714) 521-4811 FAX (714) 521-4173

SURVEYOR:
PREPARED UNDER THE DIRECTION OF:
BRIAN L. THIENES
PLS. NO. 5750
DATE: 02/15/23

CITY OF PERRIS
TENTATIVE PARCEL MAP NO. 38518
BEING A SUBDIVISION OF LOTS 1, 2, 3, 8, 9 AND 10 OF JOHNSON'S SUBDIVISION, AS PER MAP FILED IN BOOK 15, PAGE 705 OF MAPS, SAN DIEGO COUNTY RECORDS, TOGETHER WITH THAT CERTAIN UNNAMED STREET LYING SOUTH OF LOTS 1, 2 AND 3, AS ABANDONED BY RESOLUTION RECORDED JULY 19, 1960, AS INSTRUMENT NO. 64051 OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA.

TENTATIVE PARCEL MAP NO. 38518

IN THE CITY OF PERRIS, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA,



PARCEL 1
 70.604 AC. (GROSS)
 65.174 AC. (NET)

PARCEL 2
 65.560 AC. (GROSS)
 64.098 AC. (NET)

PARCEL 3
 38.951 AC. (GROSS)
 37.108 AC. (NET)

LOT A - OPEN SPACE AREA
 26.528 AC. (GROSS)
 25.391 AC. (NET)

STREET

MAPES ROAD

ROAD

ROAD

GOETZ

WATSON ROAD

ROAD

SAN JACINTO CHANNEL

NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. **Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org.** The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. For more information please contact **ALUC Planner Jackie Vega at (951) 955-0982.**

The City of Perris Planning Department should be contacted on non-ALUC issues. For more information, please contact City of Perris Planner Kenneth Phung at 951-943-5003.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to javega@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please contact Barbara Santos at (951) 955-5132.

PLACE OF HEARING: **Riverside County Administration Center**
 4080 Lemon Street, 1st Floor Board Chambers
 Riverside California

DATE OF HEARING: **June 8, 2023**

TIME OF HEARING: **9:30 A.M.**

CASE DESCRIPTION:

ZAP1029PV23 – IDI Logistics (Representative: T&B Planning) – City of Perris Case Nos. PLN22-05265 (Major Modification Review), TPM38518 (Tentative Parcel Map). A proposal to construct three industrial warehouse buildings totaling 3,343,584 square feet on three parcels on 166.36 total acres, located southerly of Mapes Street, westerly of Goetz Road, and easterly of A Street. The applicant also proposes to divide the parcel into three commercial parcels. (Airport Compatibility Zone D and E of the Perris Valley Airport Influence Area and Zone E of the March Air Reserve/Inland Port Airport Influence Area).



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC STAFF ONLY

ALUC Case Number: ZAP1029PV23

Date Submitted: 3/23/23

AIA: PERRIS VALLEY/MARCH

Zone: D&E

Public Hearing

Staff Review

Applicant

Applicant Full Name: IDI Logistics Attn: Brandon Dickens

Applicant Address: 840 Apollo St. Suite 343, El Segundo, CA 90245

Phone: (714) 915-7678 Email: brandon.dickens@idilogistics.com

Representative/ Property Owner Contact Information

Representative: Nicole Morse

Email: nmorse@tbplanning.com

Phone: (714)505-6360 ext. 126

Address: 3200 El Camino Real, Suite 100, Irvine, CA 92602

Property Owner: IDI Logistics Attn: Brandon Dickens

Email: brandon.dickens@idilogistics.com

Phone: (714) 915-7678

Address: 840 Apollo St. Suite 343, El Segundo, CA 90245

Local Jurisdiction Agency

Agency Name: City of Perris

Phone: (951) 943-5003 ext. 257

Staff Contact: Kenneth Phung

Email: kphung@cityofperris.org

Address: 11 S. D St. Perris, CA 92570

Local Agency Case No.: Major Mod PLN22-05265, TPM No. 38518

Project Location

Street Address: Southwest Corner of Mapes Rd and Goetz Rd Gross Parcel Size: 198.85 AC

Assessor's Parcel No.: 330-120-002, -003, -009, -011, -015, -016

Solar

Is the project proposing solar Panels? Yes



No



If yes, please provide solar glare study.
(only if in Zone C or higher)

Data

Site Elevation:(above mean sea level) 1424 Feet

Height of Building or structures: 51' 6" for all three buildings

What type of drainage basins are being proposed and the square footage: One detention basin (120,661 SF), two bioretention basins (6,098 SF), and earthen drainage channels (1,197,029 SF)

Notice

A. NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. REVIEW TIME: Estimated time for “staff level review” is approximately 30 days from date of submittal. Estimated time for “commission level review” is approximately 45 days from date of a complete application submittal to the next available commission hearing meeting.

C. SUBMISSION PACKAGE:

Please submit all application items DIGITALLY via USB or CD:

- Completed ALUC Application Form
- Plans Package: site plans, floor plans, building elevations, grading plans, subdivision maps
- Exhibits of change of zone, general plan amendment, specific plan amendment
- Project description of existing and proposed use

Additionally, please provide:

- ALUC fee payment (Checks made out to Riverside County ALUC)
- Gummed address labels of all surrounding property owners within a 300-foot radius of project site. (Only required if the project is scheduled for a public hearing).

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

STAFF REPORT

ADMINISTRATIVE ITEMS

5.1 Director's Approvals

- A. During the period of April 16, 2023, through May 15, 2023, as authorized pursuant to Section 1.5.2(d) of the 2004 Riverside County Airport Land Use Compatibility Plan, ALUC Director Paul Rull reviewed one non-legislative case and issued a determination of consistency.

ZAP1126FV23 (Zone D) pertains to County of Riverside Case No. CUP230003 (Conditional Use Permit), a proposal to establish a micro-brewery within an existing industrial building totaling 1,217 square feet with a tasting room on 0.24 acres, located on the northeast corner of Wealth Street and Industry way at 30826 Wealth Street. Pursuant to the French Valley Airport Land Use Compatibility Plan, the project is located within Zone D of the French Valley Airport Influence Area, which restricts non-residential average acre intensity to 150 people per acre and a maximum of 450 people in any given single acre per additional policy 2.4. The project proposes a micro-brewery within an existing industrial building totaling 1,217 square feet, including 320 square feet of manufacturing area, 350 square feet of tasting area, 144 square feet of storage area, and 403 square feet of office area, accommodating an occupancy of 28 people resulting in an average intensity of 116 people and a single acre intensity of 28 people, both of which are consistent with the Zone D intensity criteria. The elevation of Runway 14-32 at its northerly terminus is 1,347 feet above mean sea level (AMSL). At a distance of approximately 1,997 feet from the project to the nearest point on the runway, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,367 feet AMSL. The site's finished floor elevation is 1,350 feet AMSL and existing building height is 25 feet, resulting in a top point elevation of 1,375 feet AMSL. Therefore, review by the FAA Obstruction Evaluation Service would normally be required, however, the building already exists and the project does not propose increasing the height.

ALUC Director Paul Rull issued a determination of consistency for this project on May 5, 2023.

- B. Additionally, ALUC Director Paul Rull reviewed one local jurisdiction non-impact legislative cases pursuant to ALUC Resolution No. 2011-02, and issued a determination of consistency.

ZAP1072RG23 (Zone E March and Zone E Perris Valley) pertains to County of Riverside No. GPA1205 (General Plan Amendment), a proposal for The Highway 74 Community Plan (Project) consisting of General Plan Amendment (GPA) No. 1205 and a Zone Consistency Program which is proposed to master plan future development along a 6.8-mile long corridor of Highway 74 between the cities of Lake Elsinore and Perris in western Riverside County. The Project boundary encompasses approximately 2,220 acres of unincorporated land and includes portions of the Goodhope, Meadowbrook and Warm Springs Communities. As proposed, the Highway 74 Community Plan would provide for the development of residential neighborhoods of varying densities, along with commercial retail, mixed use areas, light industrial, business park, public facilities, rural, open space, and recreation areas. GPA No. 1205 will involve amendments to the existing General Plan Rural Village Land Use Overlays, Policy Areas, Foundation Components, land use designations, policies, trails, and road classifications. The Project will also include a Zone Consistency Program that will recommend parcel specific zone classifications that are consistent with the proposed land use designations. The proposed amendments do not involve changes in development standards or allowable land uses that would be in conflict with the underlying compatibility criteria in Zone E of March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan and Zone E of Perris Valley Airport Land Use Compatibility Plan. Therefore, these amendments will not impact the safety

of air navigation within both airport influence areas.

ALUC Director Paul Rull issued a determination of consistency for this project on May 4, 2023.

- C. Additionally, as authorized pursuant to ALUC Resolution No. 2015-01, as extended by Resolution No. 2020-01, ALUC Director Paul Rull reviewed one legislative cases in Zone E within March Air Reserve Base/Inland Port Airport Influence Area and issued determinations of consistency.

ZAP1551MA22 (Zone E) pertains to County Case Nos. SP00260A4 (Specific Plan Amendment), GPA210116 (General Plan Amendment), CZ210130 (Change of Zone), TTM38131 (Tentative Tract Map), a proposal to divide 52.4 acres into 201 single family residential lots located northerly of Highway 74, southerly of Alicante Drive, easterly Charina Lane, and westerly of Juniper Flats Road. The applicant also proposes to amend 5.4 acres of the site's General Plan land use designation from Medium Density Residential to Commercial Retail and amend 17.5 acres of the site from Medium Density Residential to Medium High Density Residential. The applicant also proposes to change the zoning of 17.5 acres (new PA48) from Rural Residential to Specific Plan No. 260, and to adopt amended Zoning Ordinance standards for existing PA 45 and starts for new PA's 47, 48, and 49, and define new and/or modified boundaries for PA's 45, 47, 48 and 49. The applicant also proposes to amend Specific Plan 260 1) modify the boundaries of PA 45 by removing a portion located east of Leon Road and west of Juniper Flats Road totaling 34.69 acres to create new PA 47 and PA 49. The amended PA 45 will be bordered by Leon Road to the east, Watson Road to the south and Alicante Drive to the north, will continue to have a Planning Area Land Use of Medium Density Residential (MDR), a new area of 27.3 acres, and allowable density from 3.6 to 4.2 dwelling units/acre. New PA 47 will be bound by Leon Road to the west and Juniper Flats Road to the east, will continue to have a Planning Area Land Use of Medium Density Residential (MDR), have an area of 29.5 acres, and an allowable density from 3.6 to 3.8 dwelling units/ acre. New PA 49 is located at the northwest corner of Alicante Drive and Juniper Flats Road, will have a new Planning Area Land Use designation of Commercial Retail (CR) and an area of 5.4 acres. 2) Expand the boundaries of Specific Plan No. 260 with the incorporation of 17.5 acres located south of Watson Road, east of Leon Road, and west of Juniper Flats Road for the establishment of new PA 48 with a Planning Area Land Use of Medium High Density Residential (MHDR) and allowed density of 5.1 dwelling units per acre. The project is located within Compatibility Zone E of March Air Reserve Base/Inland Port Airport Influence Area, where Zone E does not restrict residential density. Although the project is located within the March Air Reserve Base/Inland Port Airport Influence Area, the nearest runway is actually Runway 5-23 at Hemet-Ryan Airport. The elevation of Runway 5-23 at Hemet-Ryan Airport is approximately 1,499 feet above mean sea level (AMSL) at its westerly terminus. At a distance of 27,048 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures taller than 200 feet in height. The project proposes a maximum structure height of 50 feet. At a distance of 27,048 feet from the runway, FAA OES review for height/elevation was not required.

ALUC Director Paul Rull issued a determination of consistency for this project on April 25, 2023.

5.2 Update March Air Reserve Base Compatibility Use Study (CUS)
Presentation by Project Director Simon Housman or his designee.

X:\ALUC Administrative Items\Admin. 2023\Admin Item 6-8-23.doc

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



May 5, 2023

Rene Aguilar, Project Planner
Riverside County Planning Division
4080 Lemon Street, 12th Floor
Riverside CA 92501

CHAIR
Steve Manos
Lake Elsinore

VICE CHAIR
Russell Betts
Desert Hot Springs

COMMISSIONERS

John Lyon
Riverside

Steven Stewart
Palm Springs

Richard Stewart
Moreno Valley

Michelle Geller
Riverside

Vernon Poole
Murrieta

STAFF

Director
Paul Rull

Simon Housman
Jackie Vega
Barbara Santos

County Administrative Center
4080 Lemon St., 14th Floor.
Riverside, CA 92501
(951) 955-5132

www.rcaluc.org

RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW – DIRECTOR’S DETERMINATION

File No.: ZAP1126FV23
Related File No.: CUP230003 (Conditional Use Permit)
APN: 963-081-023
Airport Zone: Zone D

Dear Mr. Aguilar:

As authorized by the Riverside County Airport Land Use Commission (ALUC) pursuant to Section 1.5.2(d) of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed County of Riverside Case No. CUP230003 (Conditional Use Permit), a proposal to establish a micro-brewery within an existing industrial building totaling 1,217 square feet with a tasting room on 0.24 acres, located on the northeast corner of Wealth Street and Industry way at 30826 Wealth Street.

Pursuant to the French Valley Airport Land Use Compatibility Plan, the project is located within Zone D of the French Valley Airport Influence Area, which restricts non-residential average acre intensity to 150 people per acre and a maximum of 450 people in any given single acre per additional policy 2.4. The project proposes a micro-brewery within an existing industrial building totaling 1,217 square feet, including 320 square feet of manufacturing area, 350 square feet of tasting area, 144 square feet of storage area, and 403 square feet of office area, accommodating an occupancy of 28 people resulting in an average intensity of 116 people and a single acre intensity of 28 people, both of which are consistent with the Zone D intensity criteria.

The elevation of Runway 14-32 at its northerly terminus is 1,347 feet above mean sea level (AMSL). At a distance of approximately 1,997 feet from the project to the nearest point on the runway, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,367 feet AMSL. The site’s finished floor elevation is 1,350 feet AMSL and existing building height is 25 feet, resulting in a top point elevation of 1,375 feet AMSL. Therefore, review by the FAA Obstruction Evaluation Service would normally be required, however, the building already exists and the project does not propose increasing the height.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2007 French Valley Airport Land Use Compatibility Plan, as amended in 2011, provided that the County of Riverside applies the following recommended conditions:

CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.

AIRPORT LAND USE COMMISSION

3. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, outdoor production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Highly noise-sensitive outdoor nonresidential uses.
 - (f) Any use which results in a hazard to flight, including physical (e.g., tall objects), visual, and electronic forms of interference with the safety of aircraft operations.
3. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property and be recorded as a deed notice.
4. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the stormwater basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

AIRPORT LAND USE COMMISSION

5. This project has been evaluated as consisting of a micro-brewery within an existing industrial building totaling 4,969 square feet with a tasting room. Any increase in building area, change in use to any higher intensity use, change in building location or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.

If you have any questions, please contact me at (951) 955-6893.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Paul Rull, ALUC Director

Attachments: Notice of Airport in Vicinity

cc: French Valley Brewing Company, Inc. (applicant/representative)
IPS Controllers (property owner)
Angela Jamison, County Airports Manager
ALUC Case File

X:\AIRPORT CASE FILES\French Valley\ZAP1126FV23\ZAP1126FV26.LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS

PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



Legend

- Compatibility Zones**
- Airport Influence Area Boundary
 - Zone A
 - Zone B1
 - Zone B2
 - Zone C
 - Zone D
 - Zone E
- Boundary Lines**
- Airport Property Line
 - City Limits
 - ★ Height Review Overlay Zone

Note

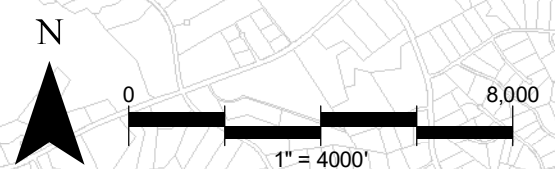
Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

See Chapter 2, Table 2A from compatibility criteria associated with this map.

Riverside County
Airport Land Use Commission

Riverside County
Airport Land Use Compatibility Plan
Policy Document

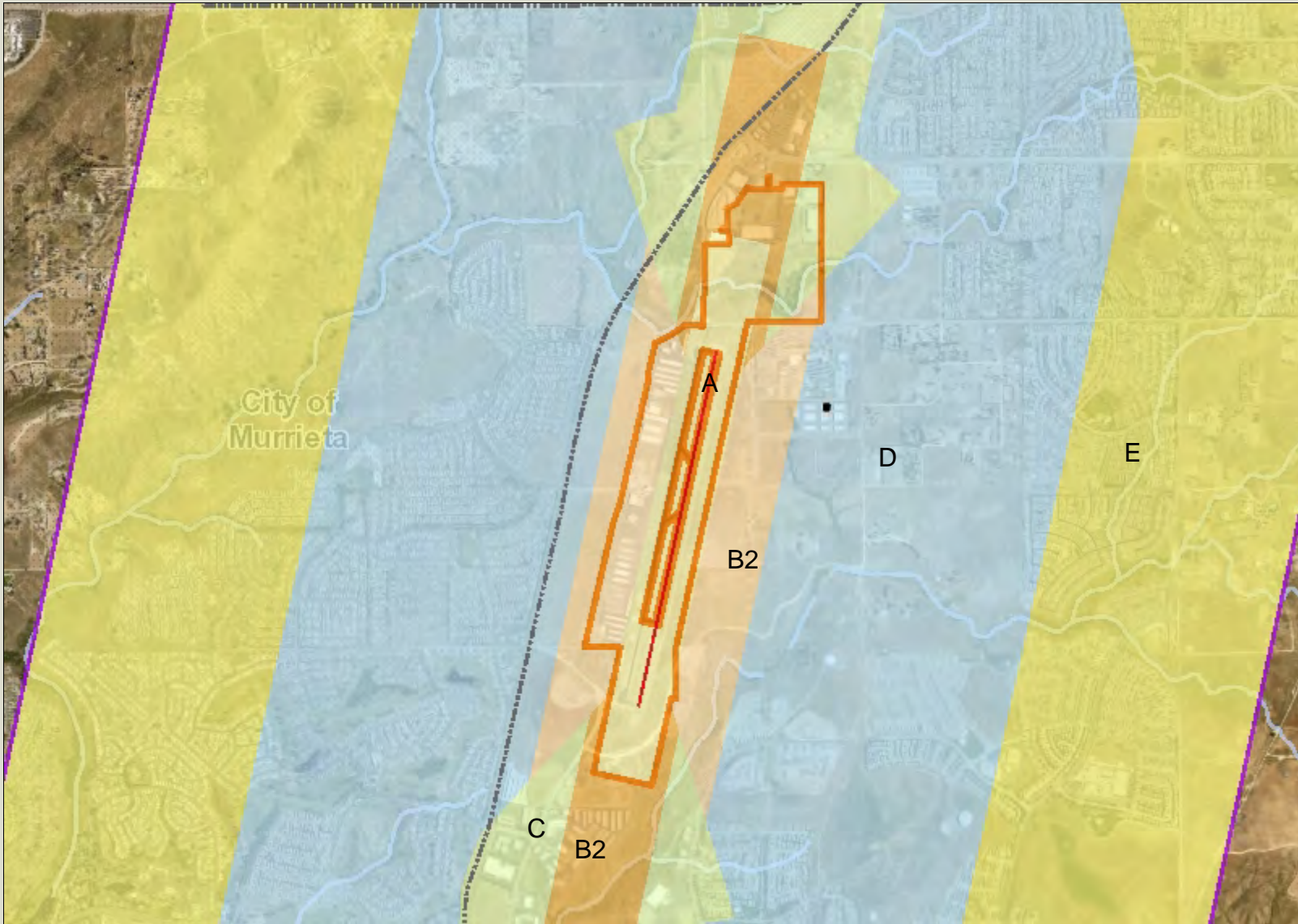
 (April 2010)



Map FV-1

Compatibility Map
French Valley Airport

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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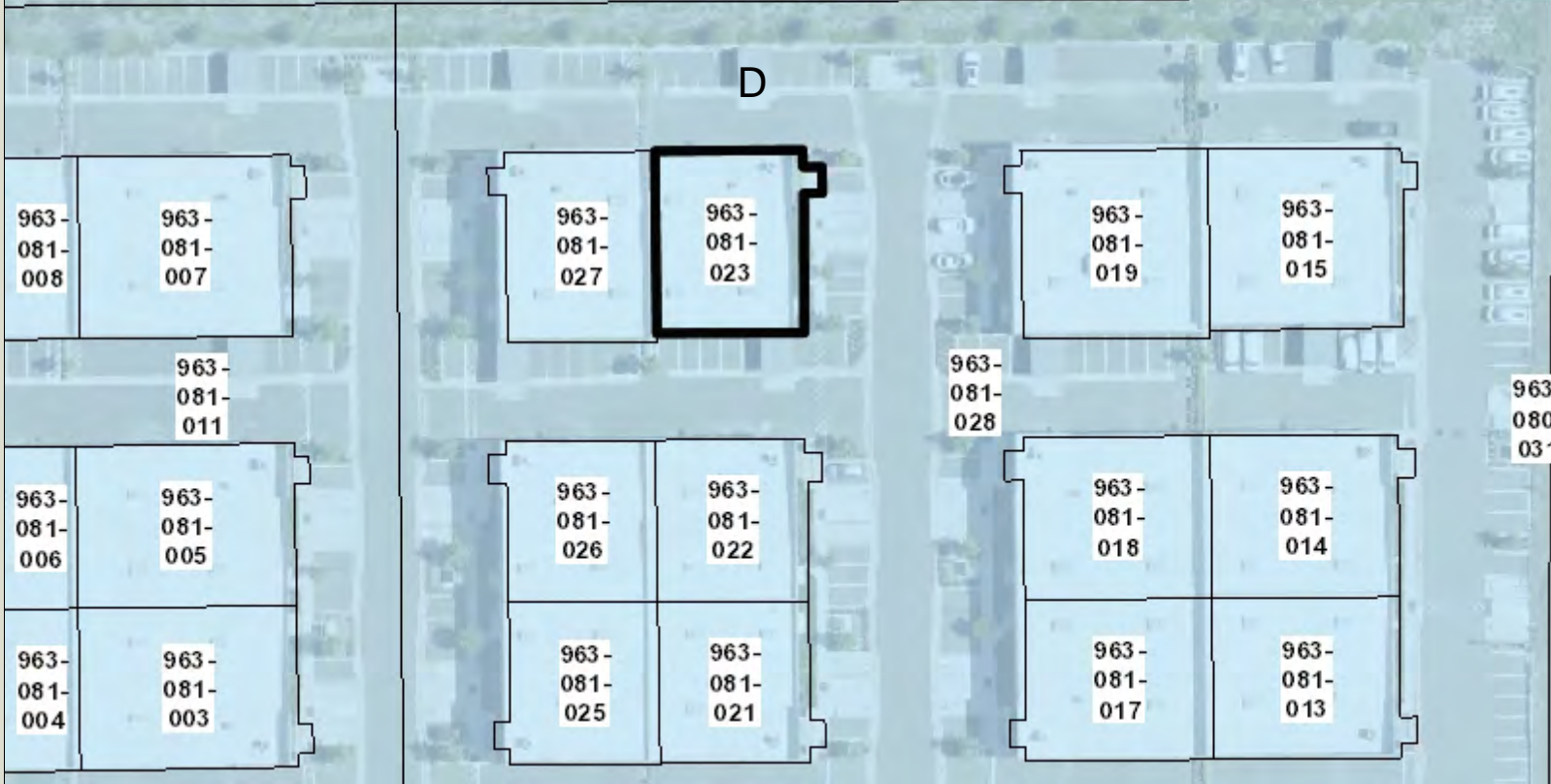
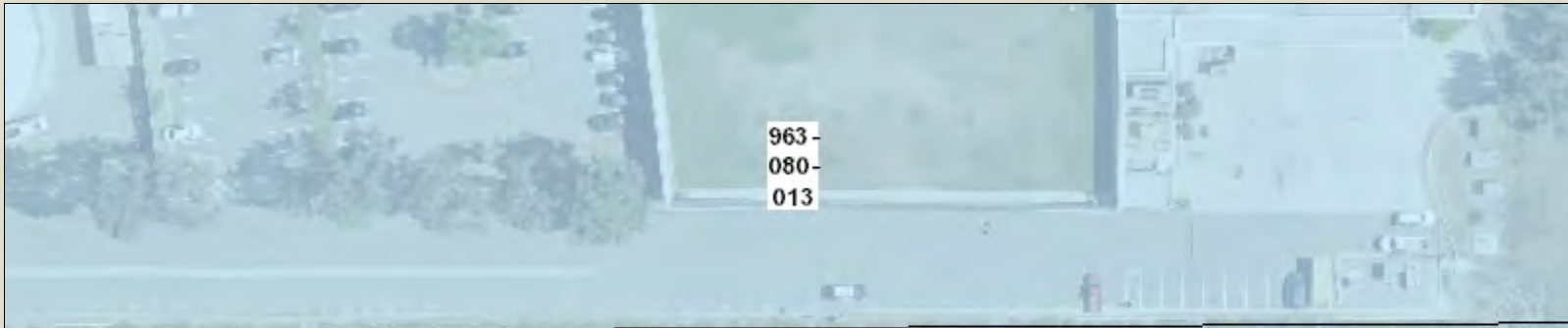
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REPORT PRINTED ON... 4/28/2023 10:36:16 AM

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Notes

Map My County Map



Legend

- Parcel APNs
- Parcels
- Runways
- Airports
- Airport Influence Areas

Airport Compatibility Zones

- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3



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Notes

Map My County Map



Legend

- Parcel APNs
- Parcels
- County Centerline Names
- County Centerlines
- Blueline Streams
- ⋯ City Areas
- World Street Map



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Notes

0 96 192 Feet

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Map My County Map



- Legend**
- County Centerline Names
 - County Centerlines
 - Blueline Streams
 - City Areas
 - World Street Map



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Notes

0 770 1,539 Feet

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Map My County Map



Legend

-  Parcels
-  County Centerline Names
-  County Centerlines
-  Blueline Streams
-  City Areas
-  World Street Map



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0 385 770 Feet

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Notes

Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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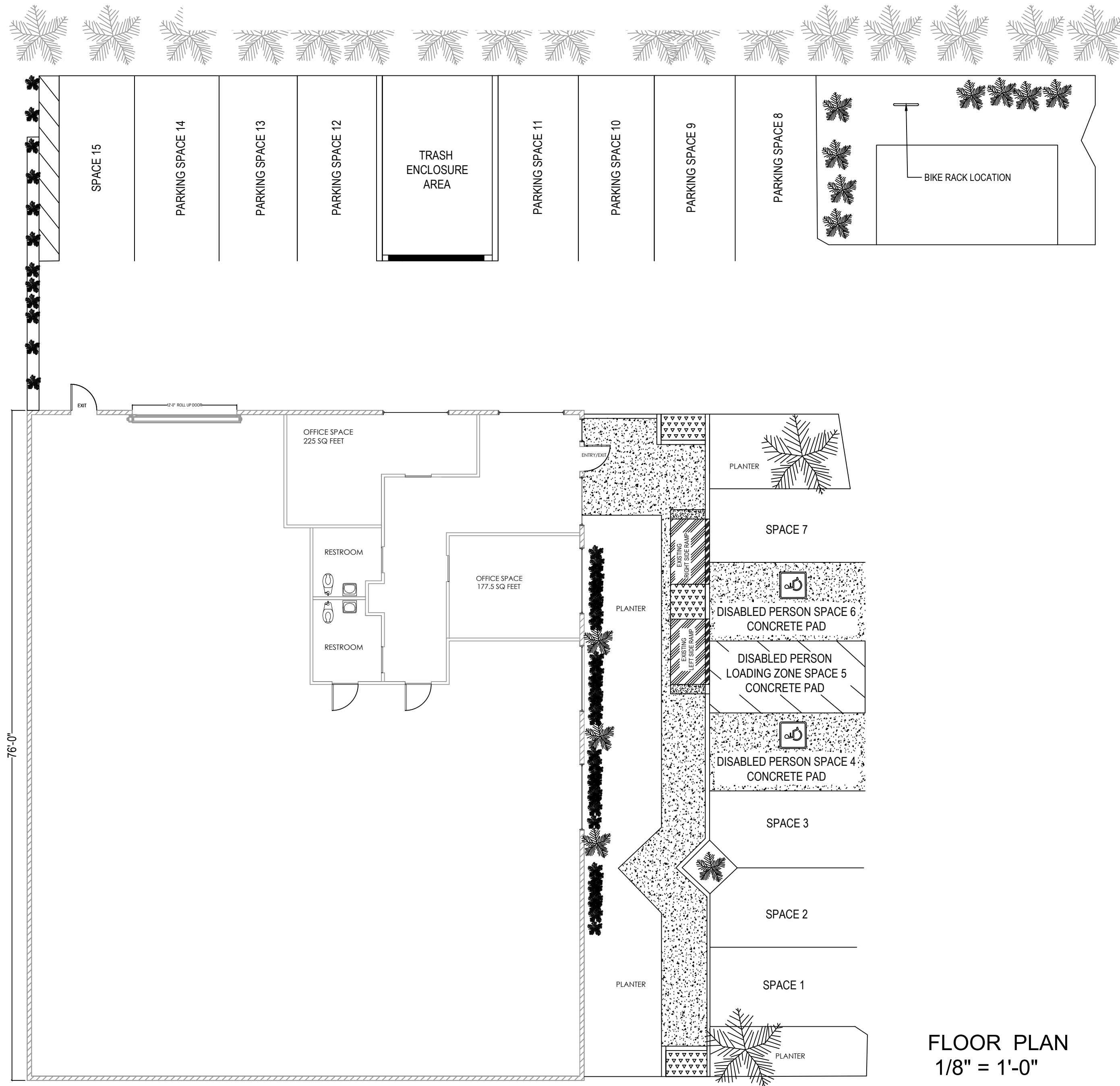
Notes



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ORIGINAL FLOOR PLAN

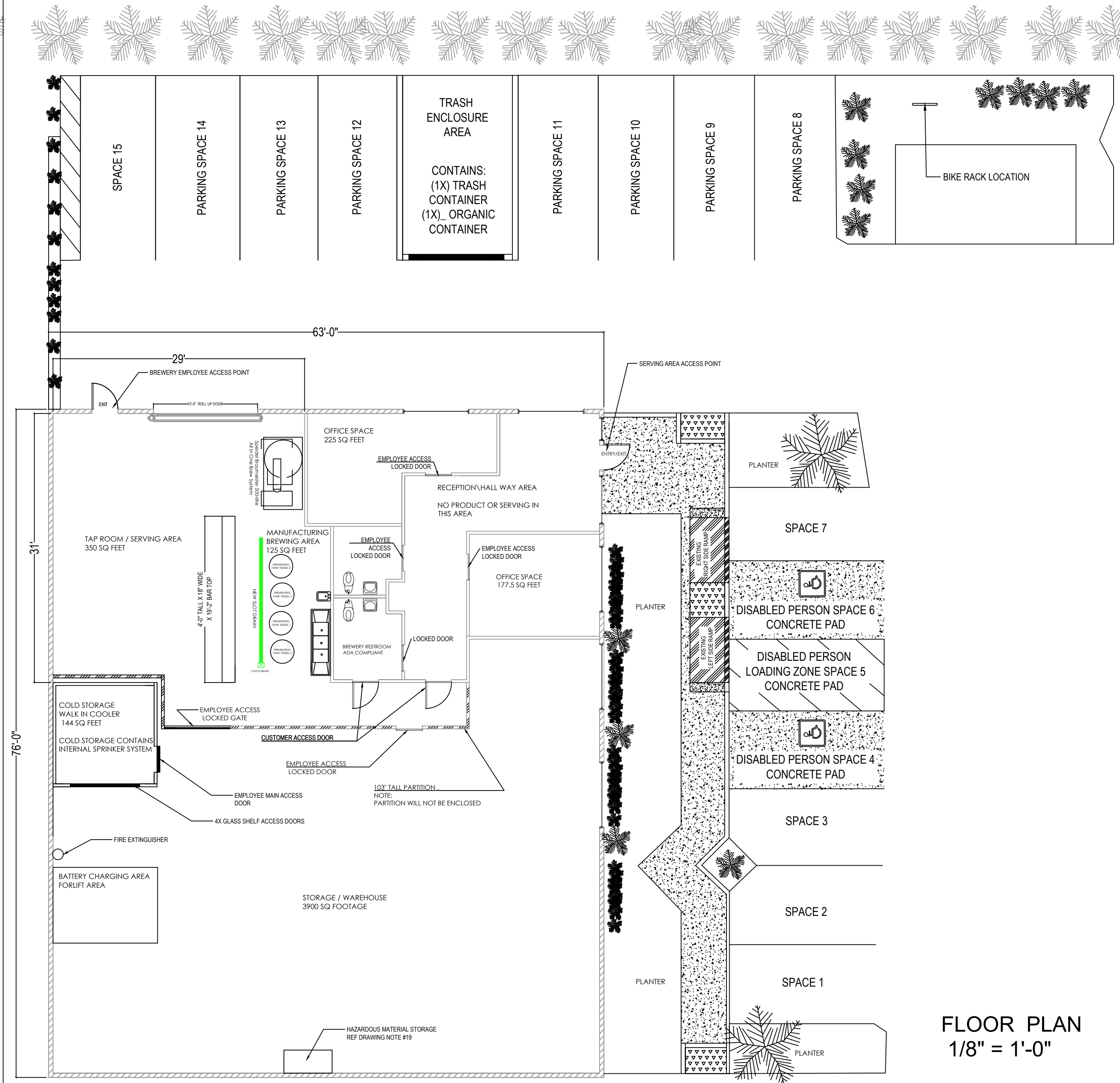


FLOOR PLAN
1/8" = 1'-0"

DRAWING NOTES:

- BUILDING GROSS SQ FOOTAGE: 4,969
- NO EXTERIOR CHANGES WERE MADE TO FEATURES, ADA ACCESS, OR PARKING LOT. FROM ORIGINAL BUILDING CONSTRUCTION, NOR ARE THEY BEING PROPOSE.
- NO CHANGES TO THE BUILDING CONDOMINIUM ASSOCIATION LIGHTING NOR ANY ADDITIONAL LIGHTS BEING PROPOSED
- 15 PARKING SPACES ALLOCATED TO FRENCH VALLEY BREWING
- ZONE D. MANUFACTURING
- NO LOADING BAYS OR DOCKS, GROUND ACCESS ONLY.
- ROLL UP DOORS TO ONLY BE USED FOR SHIPPING AND RECEIVING AND NOT USED FOR PUBLIC ENTRANCE
- ALL DOORS TO REMAIN THE SAME AS BUILD CONDITION, THE CUSTOMER RESTROOM WITH TWO DOOR WILL HAVE 1 DOOR REMAINING LOCKED BECAUSE OF THIS

PROPOSED FLOOR PLAN



FLOOR PLAN
1/8" = 1'-0"

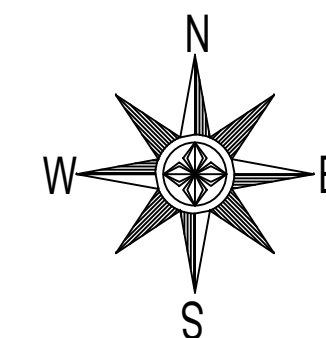
APPLICANT:
FRENCH VALLEY BREWING
COMPANY, INC
C/O DAN WOOLSTENHULME
30826 WEALTH ST.
MURRIETA CA, 92563
PHONE: 951-541-3444

LAND OWNER:
WRW20 LLC.
C/O CHUCK WOOLSTENHULME
30826 WEALTH ST.
MURRIETA CA, 92563
PHONE: 909-518-8530

EXHIBIT PREPARER:
DANNY MARTINS
3937 MASONBORO LOOP RD
WILMINGTON NC, 28409
PHONE: 951-200-9226

LEGEND	
EXISTING WALLS	CONCRETE
NEW PARTITION	EXISTING WINDOW
NEW SLOT DRAIN WITH CATCH BASIN	

WALKWAY LEGEND	
CONCRETE WALK WAY	TRUNCATED DOME BUMP PAD
FLARED CURB	SIDE RAMP



FRENCH VALLEY BREWING
30826 WEALTH ST
MURRIETA, CA 92563

PROPRIETARY AND CONFIDENTIAL
THE INFORMATION CONTAINED IN THIS DRAWING IS THE SOLE PROPERTY OF FRENCH VALLEY BREWING. ANY REPRODUCTION IN PART OR AS A WHOLE WITHOUT THE WRITTEN PERMISSION OF FRENCH VALLEY BREWING IS PROHIBITED.

CONFIDENTIAL
NOT FOR CONSTRUCTION

EXHIBIT AMENDMENT BLOCK	
DATE	DESCRIPTION
4/08/2023	DRAWING CREATED PER SITE PLAN CORRECTIONS
4/18/2023	UPDATED DRAWING NOTES/ WALL PARTITION/ LEGEND NOTES
REV #	
REV 1.00	
REV 2.00	

PROJ. NUMBER	5928
PROJ. MANAGER	PTM

FRENCH VALLEY BREWING

BY:	DTM
SCALE:	96:1
SIZE:	D

DRAWING NUMBER
CUP23003-EX-PR
FRENCH VALLEY BREWERY
CONDITIONAL USE,
MURRIETA, CA

FRENCH VALLEY BREWING
30826 WEALTH ST
MURRIETA, CA 92563

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REV #	DATE	DESCRIPTION
REV 1.00	4/08/2023	DRAWING CREATED PER SITE PLAN CORRECTIONS
REV 2.00	4/18/2023	UPDATED DRAWING NOTES; WALL PARTITION; LEGEND NOTES

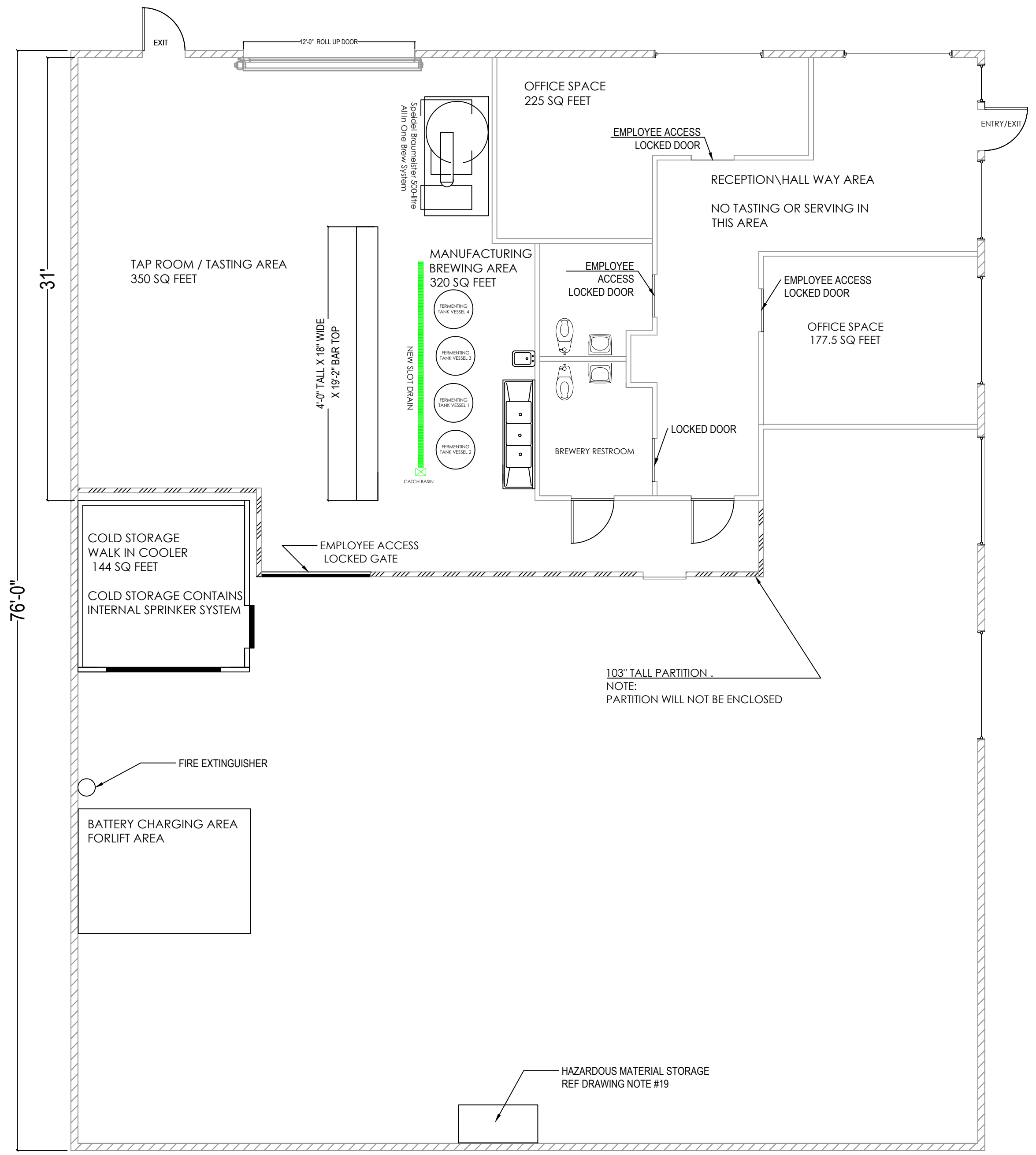
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PROJ. MANAGER PTM

FRENCH VALLEY BREWING

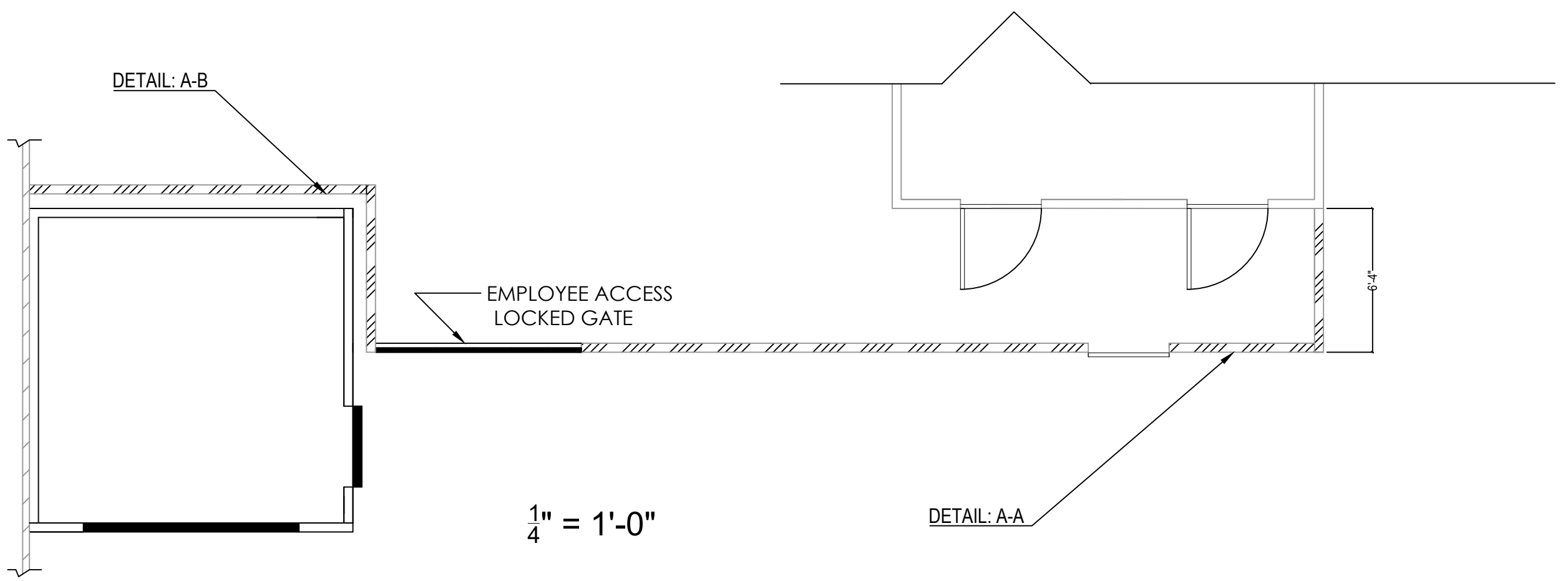
BY: DTM
SCALE: 96:1

SIZE: D
DRAWING NUMBER

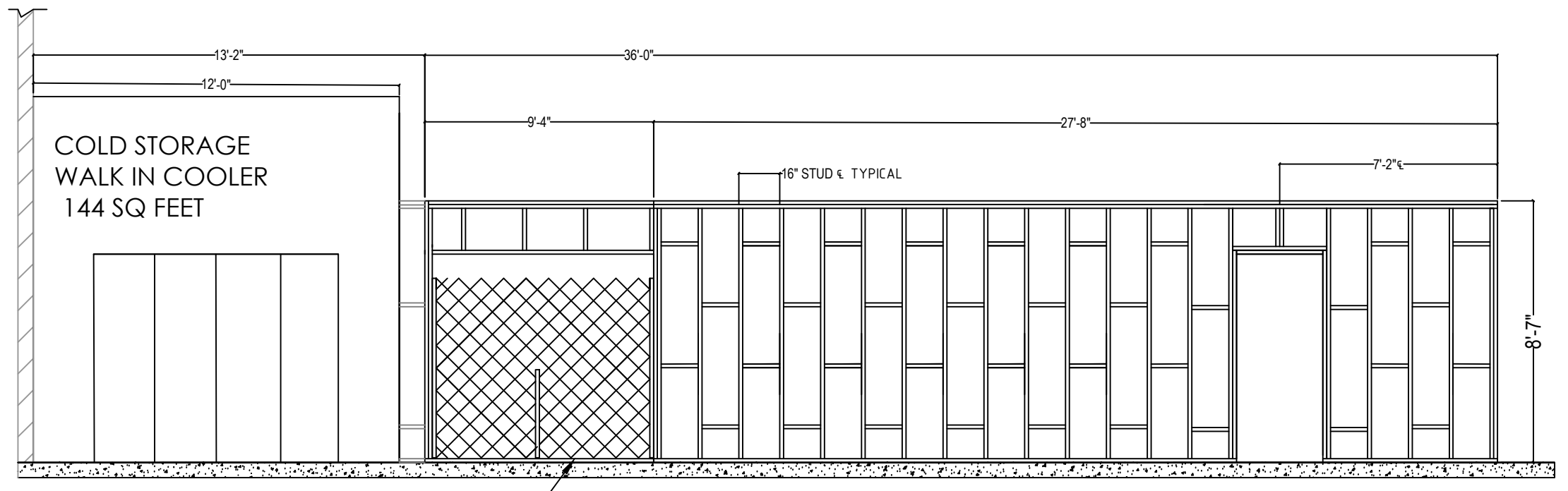
CUP23003 -IN-PAR
FRENCH VALLEY BREWERY
CONDITIONAL USE,
MURRIETA, CA



FLOOR PLAN
3/16" = 1'-0"



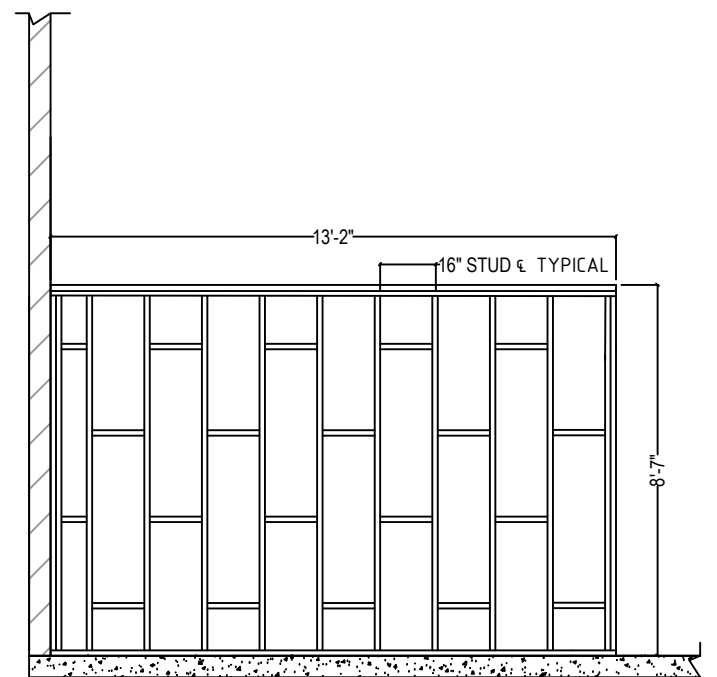
1" = 1'-0"



WALL PARTITION: 8'-7" TALL X 36'-0" LENGTH
MATERIAL: USING 2 X 4 METAL STUD FRAME ON 16" CENTERS
WRAP WITH 1/2" DRYWALL, FINISHED & PAINTED

FRONT VIEW A
DETAIL A-A
1" = 1'-0"

DRAWING NOTES:
PARTITION WILL NOT BE ENCLOSED

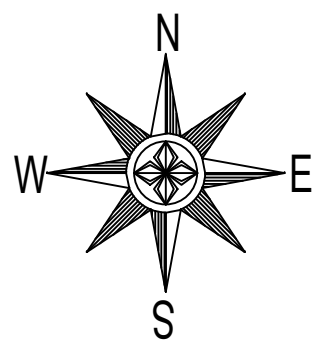


FRONT VIEW A-1
DETAIL A-B
1" = 1'-0"

WALL PARTITION: 8'-7" TALL X 13'-2" LENGTH
MATERIAL: USING 2 X 4 METAL STUD FRAME ON 16" CENTERS
WRAP WITH 1/2" DRYWALL, FINISHED & PAINTED

LEGEND

EXISTING WALLS	—
CONCRETE	▨
2X4 WALL	▤
NEW PARTITION	▥
EXISTING WINDOW	◻
NEW WINDOW	◻



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



May 4, 2023

Andrew Svitek, Project Planner
County of Riverside Planning Department
4080 Lemon Street, 12th Floor
Riverside CA 92501

CHAIR
Steve Manos
Lake Elsinore

VICE CHAIR
Russell Betts
Desert Hot Springs

COMMISSIONERS

John Lyon
Riverside

Steven Stewart
Palm Springs

Richard Stewart
Moreno Valley

Michael Geller
Riverside

Vernon Poole
Murrieta

STAFF

Director
Paul Rull

Simon Housman
Jackie Vega
Barbara Santos

County Administrative Center
4080 Lemon St., 14th Floor.
Riverside, CA 92501
(951) 955-5132

RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW – DIRECTOR'S DETERMINATION

File No.: ZAP1072RG23
Related File No.: GPA1205 (General Plan Amendment)
Compatibility Zone: March (Zone E), Perris Valley (Zone E)
APN: Countywide

Dear Mr. Svitek,

As authorized by the Riverside County Airport Land Use Commission (ALUC) pursuant to its Resolution No. 2011-02, as ALUC Director, I have reviewed County of Riverside General Plan Amendment (GPA1205), a proposal for The Highway 74 Community Plan (Project) consisting of General Plan Amendment (GPA) No. 1205 and a Zone Consistency Program which is proposed to master plan future development along a 6.8-mile long corridor of Highway 74 between the cities of Lake Elsinore and Perris in western Riverside County. The Project boundary encompasses approximately 2,220 acres of unincorporated land and includes portions of the Goodhope, Meadowbrook and Warm Springs Communities. As proposed, the Highway 74 Community Plan would provide for the development of residential neighborhoods of varying densities, along with commercial retail, mixed use areas, light industrial, business park, public facilities, rural, open space, and recreation areas. GPA No. 1205 will involve amendments to the existing General Plan Rural Village Land Use Overlays, Policy Areas, Foundation Components, land use designations, policies, trails, and road classifications. The Project will also include a Zone Consistency Program that will recommend parcel specific zone classifications that are consistent with the proposed land use designations.

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The proposed amendments do not involve changes in development standards or allowable land uses that would be in conflict with the underlying compatibility criteria in Zone E of March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan and Zone E of Perris Valley Airport Land Use Compatibility Plan. Therefore, these amendments will not impact the safety of air navigation within both airport influence areas.

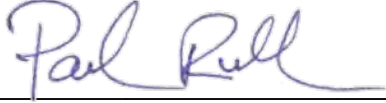
As ALUC Director, I hereby find the above-referenced project **CONSISTENT**, with the 2011 Perris Valley Airport Land Use Compatibility Plan and the 2014 March Air Reserve Base Airport Land Use Compatibility Plan.

This determination of consistency relates to airport compatibility issues and does not necessarily constitute an endorsement of the proposed amendment.

AIRPORT LAND USE COMMISSION

If you have any questions, please contact me at (951) 955-6893.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A handwritten signature in blue ink that reads "Paul Rull". The signature is written in a cursive style with a horizontal line underneath it.

Paul Rull, ALUC Director

cc: ALUC Case File

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- GPA No. 1075, BOS RSLN 2011-156, 10/18/11
- GPA No. 743, BOS RSLN 2015-214,09/22/15
- GPA Nos. 985, 988, BOS RSLN 2016-098, 03/29/16
- GPA No. 1156, 1166 BOS RSLN 2017-001, 04/11/17;
- GPA No. 1146, BOS RSLN 2019-050, 04/16/19;
- GPA No. 200001, BOS RSLN 2021-108, 06/30/21
- [GPA No. 1205, BOS RSLN 2023-###, DATE HERE](#)

- GPA No. 1120, BOS RSLN 2014-222, 11/24/14
- GPA No. 960, BOS RSLN 2015-260,12/08/15
- GPA No. 1122, BOS RSLN 2016-234, 12/06/16
- GPA No. 1223, BOS RSLN 2018-118, 06/26/18;
- GPA No. 1208, BOS RSLN 2019-161, 08/04/20;
- GPA No. 190006, BOS RSLN 2021-183; 09/28/21

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Vision Summary

The County of Riverside General Plan and Area Plans have been shaped by the RCIP Vision. Following is a summary of the Vision Statement that includes many of the salient points brought forth by the residents of Elsinore Area Plan as well as the rest of the County of Riverside. The RCIP Vision reflects the County of Riverside in the year 2020. ~~So, fast forward yourself to 2020 and here is what it will be like and beyond.~~

"Riverside County is a family of special communities in a remarkable environmental setting."

~~It is now the year 2020. This year (incidentally, also a common reference to clear vision), is an appropriate time to check our community vision.~~ Twenty plus years have passed since we took an entirely new look at how the County of Riverside was evolving. Based on what we saw, we set bold new directions for the future. As we now look around and move through Riverside County, the results are notable. They could happen only in response to universal values strongly held by the people. Some of those values are:

- Real dedication to a sense of community;
- Appreciation for the diversity of our people and places within this expansive landscape;
- Belief in the value of participation by our people in shaping their communities;
- Confidence in the future and faith that our long term commitments will pay off;
- Willingness to innovate and learn from our experience;
- Dedication to the preservation of the environmental features that frame our communities;
- Respect for our differences and willingness to work toward their resolution;
- Commitment to quality development in partnership with those who help build our communities;
- The value of collaboration by our elected officials in conducting public business.

Those values and the plans they inspired have brought us a long way. True, much remains to be done. But our energies and resources are being invested in a unified direction, based on the common ground we have affirmed many times during the last 20 years. Perhaps our achievements will help you understand why we believe we are on the right path.

Population Growth

The almost doubling of our population in only 20 years has been a challenge, but we have met it by focusing that growth in areas that are well served by public facilities and services or where they can readily be provided. Major transportation corridors serve our communities and nearby open space preserves help define them. Our growth focus is on quality, not quantity. That allows the numbers to work for us and not against us. We enjoy an unprecedented clarity regarding what areas must not be developed and which ones should be developed. The resulting pattern of growth concentrates development in key areas rather than spreading it uniformly throughout the County of Riverside. Land is used more efficiently, communities operate at more of a human scale, and transit systems to supplement the automobile are more feasible. In fact, the customized Oasis transit system now operates quite successfully in several cities and communities.

Our Communities and Neighborhoods

Our choices in the kind of community and neighborhood we prefer are almost unlimited here. From sophisticated urban villages to quality suburban neighborhoods to spacious rural enclaves, we have them all. If you are like most of us, you appreciate the quality schools and their programs that are the centerpiece of many of our neighborhoods. Not only have our older communities matured gracefully, but we boast several new communities as well. They prove that quality of life comes in many different forms.

Housing

We challenge you to seek a form of housing or a range in price that does not exist here. Our housing choices, from rural retreat to suburban neighborhood to exclusive custom estate are as broad as the demand for housing requires. Choices include entry level housing for first time buyers, apartments serving those not now in the buying market, seniors' housing, and world class golf communities. You will also find smart housing with the latest in built-in technology as well as refurbished historic units. The County of Riverside continues to draw people who are looking for a blend of quality and value.

Transportation

It is no secret that the distances in the vast County of Riverside can be a bit daunting. Yet, our transportation system has kept pace amazingly well with the growth in population, employment and tourism and their demands for mobility. We are perhaps proudest of the new and expanded transportation corridors that connect growth centers throughout the County of Riverside. They do more than provide a way for people and goods to get where they need to be. Several major corridors have built-in expansion capability to accommodate varied forms of transit. These same corridors are designed with a high regard for the environment in mind, including providing for critical wildlife crossings so that our open spaces can sustain their habitat value.

Conservation and Open Space Resources

The often-impassioned conflicts regarding what lands to permanently preserve as open space are virtually resolved. The effort to consider our environmental resources, recreation needs, habitat systems, and visual heritage as one comprehensive, multi-purpose open space system has resulted in an unprecedented commitment to their preservation. In addition, these spaces help to form distinctive edges to many of our communities or clusters of communities. What is equally satisfying is that they were acquired in a variety of creative and equitable ways.

Elsinore Area Plan

Air Quality

It may be hard to believe, but our air quality has actually improved slightly despite the phenomenal growth that has occurred in the region. Most of that growth, of course, has been in adjacent counties and we continue to import their pollutants. We are on the verge of a breakthrough in technical advances to reduce smog from cars and trucks. Not only that, but our expanded supply of jobs reduces the need for people here to commute as far as in the past.

Jobs and Economy

In proportion to population, our job growth is spectacular. Not only is our supply of jobs beyond any previously projected level, it has become quite diversified. Clusters of new industries have brought with them an array of jobs that attract skilled labor and executives alike. We are particularly enthusiastic about the linkages between our diversified business community and our educational system. Extensive vocational training programs, coordinated with businesses, are a constant source of opportunities for youth and those in our labor force who seek further improvement.

Agricultural Lands

Long a major foundation of our economy and our culture, agriculture remains a thriving part of the County of Riverside. While we have lost some agriculture to other forms of development, other lands have been brought into agricultural production. We are still a major agricultural force in California and compete successfully in the global agricultural market.

Educational System

Quality education, from pre-school through graduate programs, marks the County of Riverside as a place where educational priorities are firmly established. A myriad of partnerships involving private enterprise and cooperative programs between local governments and school districts are in place, making the educational system an integral part of our communities.

Plan Integration

The coordinated planning for multi-purpose open space systems, community based land use patterns, and a diversified transportation system has paid off handsomely. Integration of these major components of community building has resulted in a degree of certainty and clarity of direction not commonly achieved in the face of such dynamic change.

Financial Realities

From the very beginning, our vision included the practical consideration of how we would pay for the qualities our expectations demanded. Creative, yet practical financing programs provide the necessary leverage to achieve a high percentage of our aspirations expressed in the updated RCIP.

Intergovernmental Cooperation

As a result of the necessary coordination between the County of Riverside, the cities and other governmental agencies brought about through the RCIP, a high degree of intergovernmental cooperation and even partnership is

now commonplace. This way of doing public business has become a tradition and the County of Riverside is renowned for its many model intergovernmental programs.

Introduction

Throughout the Area Plan, special features have been included to enhance the readability and practicality of the information provided. Look for these elements:



Quotes: quotations from the RCIP Vision or individuals involved or concerned with Riverside County.



Factoids: interesting information about Riverside County that is related to the element



References: contacts and resources that can be consulted for additional information



Definitions: clarification of terms and vocabulary used in certain policies or text.

It doesn't matter whether you whiz by on Interstate 15 or wind your way down the spectacular face of the Santa Ana Mountains on [State Route/Highway 74](#); the eye cannot avoid taking in Lake Elsinore. From the I-15 you also get a bonus in the form of the precipitous slope of the mountains; from the 74 you gaze out over hills, towns and valleys stretching far into the distance. As if that was not enough, there is even the man-made Canyon Lake off to the northeast, capturing waters from the San Jacinto River. The richness of this special place isn't just in its visual qualities. It is also a collection of unique communities as well as home to a remarkable variety of natural species. The Elsinore area is a truly unique human and natural habitat within a county that encompasses many notable environments.

The Elsinore Area Plan doesn't just provide a description of the location, physical characteristics, and special features here. It contains a Land Use Plan, statistical summaries, policies, and accompanying exhibits that allow anyone interested in the continued prosperity of this distinctive area to understand the physical, environmental and regulatory characteristics that make this such a unique area. Background information also provides insights that help in understanding the issues that require special focus here and the reasons for the more localized policy direction found in this document.

Each section of the Area Plan addresses critical issues facing Elsinore. Perhaps a description of these sections will help in understanding the organization of the Area Plan as well as appreciating the comprehensive nature of the planning process that led to it. The Location section explains where the Area Plan fits with what is around it and how it relates to the cities that impact it. Physical features are described in a section that highlights the planning area's communities, surrounding environment and natural resources. This leads naturally to the Land Use Plan section, which describes the land use system guiding development at both the countywide and area plan levels.

While a number of these designations reflect the unique features found only in Elsinore, a number of special policies are still necessary to address unique situations. The Policy Areas section presents these additional policies. Land use related issues are addressed in the Land Use section. The Plan also describes relevant transportation issues, routes and modes of transportation in the Circulation section. The key to understanding the valued open space network is described in the Multipurpose Open Space section. There are, of course, both natural and manmade hazards to consider, and they are spelled out in the Hazards section.

Elsinore Area Plan

A Special Note on Implementing the Vision

The preface to this area plan is a summary version of the Riverside County Vision. That summary is, in turn, simply an overview of a much more extensive and detailed Vision of Riverside County two decades or more into the future. This area plan, as part of the Riverside County General Plan, is one of the major devices for making the Vision a reality.

No two area plans are the same. Each represents a unique portion of the incredibly diverse place known as Riverside County. While many share certain common features, each of the plans reflects the special characteristics that define its area's unique identity. These features include not only physical qualities, but also the particular boundaries used to define them, the stage of development they have reached, the dynamics of change expected to affect them, and the numerous decisions that shape development and conservation in each locale. That is why the Vision cannot and should not be reflected uniformly.

Policies at the General Plan and Area Plan levels implement the Riverside County Vision in a range of subject areas as diverse as the scope of the Vision itself. The land use pattern contained in this area plan is a further expression of the Vision as it is shaped to fit the terrain and the conditions in the Elsinore area.

To illustrate how the Vision has shaped this area plan, the following highlights reflect certain strategies that link the Vision to the land. This is not a comprehensive enumeration; rather, it emphasizes a few of the most powerful and physically tangible examples.

Pattern of Development and Open Space. The Plan intensifies and mixes uses at nodes adjacent to transportation corridors, more accurately reflects topography and natural resources in the Gavilan and Sedco Hills with appropriate land use designations, and avoids high intensity development in natural hazard areas. Land use densities step down into areas constrained by natural features, resources or habitats, or remote from transportation facilities. Existing communities and neighborhoods retain their character and are separated from one another by lower intensity land use designations where possible.

Watercourses. Temescal Wash is a major influence on the character of the northern portion of the Area Plan, traversing it from northwest to southeast and flowing around Lee Lake and adjacent to Interstate 15. Land use designations adjacent to the Wash reflect a desire to buffer it from development so that its scenic and natural resource values are retained. Murrieta Creek, which flows adjacent to Palomar Street in Wildomar, has also been illustrated as a watercourse.

Data in this area plan is current as of April 16, 2019. Any General Plan amendments approved subsequent to that date are not reflected in this area plan and must be supported by their own environmental documentation. A process for incorporating any applicable portion of these amendments into this area plan is part of the General Plan Implementation Program.



Unincorporated land is all land within the County that is not within an incorporated city or an Indian Nation. Generally, it is subject to policy direction and under the land use authority of the Board of Supervisors. However, it may also contain state and federal properties that lie outside of Board authority.

Location

The strategic location of this area is clearly evident in Figure 1, Location. Because of the access provided by [State Route/Highway 74](#) over the Santa Ana Mountains, Elsinore is a gateway to the west. It is also an important north/south link in the western flank of Riverside County. One looks outward toward five area plans that constitute a major portion of the vast development potential in western Riverside County. Starting to the south and moving counter-clockwise, we find the adjacent Southwest Area Plan, and the plans for Sun City/Menifee Valley, Mead Valley, Lake Mathews/Woodcrest and Temescal Canyon. The cities of Lake Elsinore, Wildomar and Canyon Lake are core communities here. Murrieta approaches from the south and Perris from the northeast, but neither extend into this planning area. Moreover, the Elsinore planning area borders on both San Diego County to the south and Orange County to the west. These relationships can be better visualized by reference to Figure 1, Location, which also depicts the unincorporated places that have a strong local identity. As a framework for these locales, some of the more prominent physical features are also shown on this exhibit.

Features

The Riverside County Vision builds heavily on the value of its remarkable environmental setting. That certainly applies here as well. This section describes the setting, features and functions that are unique to the Elsinore Area Plan. These defining characteristics are shown on Figure 2, Physical Features.

Setting



The San Jacinto River meanders over 40 miles through Riverside County, beginning at Lake Hemet in the San Jacinto Mountains and terminating at Lake Elsinore.

Much of the Elsinore Area Plan is situated within a valley, running from northwest to southeast, framed by the Santa Ana and Elsinore Mountains on the west and the Gavilan and Sedco Hills on the east. Lake Elsinore, which is the largest natural lake in Southern California, covering about 3,000 surface acres, is a centerpiece in the valley. Lake Elsinore is the terminus of the San Jacinto River, which is regulated by the Railroad Canyon dam and generally stabilized at an elevation of approximately 1,230 feet. The Lake is fed by the San Jacinto River and underground springs and is drained by the Temescal Wash to the north, flowing eventually into the Santa Ana River. Murrieta Creek, which eventually drains into the Santa Margarita River, starts just south of Lake Elsinore. Lake Elsinore, Canyon Lake, the San Jacinto River, Temescal Wash, and Murrieta Creek provide a distinctive pattern of lakes and watercourses throughout the valley floor and the settlements here are significantly shaped by the richness of both waterways and the widely varied topography. It is truly a remarkable setting.

Elsinore Area Plan

Unique Features

Cleveland National Forest

The Cleveland National Forest forms the western boundary of the area and encompasses large portions of the Santa Ana and Elsinore Mountains. This area is characterized by natural open space and outdoor recreational uses with pockets of rural residential and wilderness oriented visitor serving uses scattered along [State Route Highway 74](#). Private inholdings within the Forest boundary are developed with limited residential and commercial uses.

Temescal Wash

The Temescal Wash creates an impressive swath pinched between the Gavilan Hills and the Santa Ana Mountains. Although dry most of the year, the wash serves as an outlet for Lake Elsinore and eventually drains into the Santa Ana River. While the wash runs in a generally northwest/southeast direction, it also provides a critical perpendicular linkage for animals between the mountain and hill habitats on either side. That is why the wash plays such an important role in the Western Riverside County Multiple Species Habitat Conservation Plan.

Unique Communities

Commented [SA1]: Add Good Hope?

Meadowbrook

Meadowbrook, an Unincorporated Community recognized by the Local Agency Formation Commission (LAFCO) in 1997, is situated in the northeastern portion of the Area Plan immediately north and east of presently undeveloped portions of the City of Lake Elsinore. This community includes some commercial and light industrial uses focused along [State Route Highway 74](#), the central transportation spine within the community. However, Meadowbrook is generally characterized by very low density residential development and vacant properties set amid rolling hills. Community residents have expressed interest in economic development through implementation of [a Rural Village Land Use Overlay](#) [the Highway 74 Policy Area](#).



A Community of Interest (COI) is a study area designated by LAFCO within unincorporated territory that may be annexed to one or more cities or special districts, incorporated as a new city, or designated as an **Unincorporated Community (UC)** within two years of status obtainment.

Designation of an area as a UC may require removal from a municipal sphere of influence since the two designations are mutually exclusive.

Warm Springs

Warm Springs, a Community of Interest recognized by LAFCO, forms a portion of the northern boundary of the Elsinore Area Plan. The northerly portion of this community is set in the Gavilan Hills. A strip along the north edge of this area, along the border of the Lake Mathews/Woodcrest Area Plan, is within the sphere of influence of the relatively distant City of Riverside. This area is generally characterized by rural uses set along steep slopes. Development is concentrated adjacent to Interstate 15 and in a focused area along [State Route Highway 74](#) adjacent to the City of Lake Elsinore.

Horsethief Canyon

Horsethief Canyon is located in the northwestern corner of the plan area. This emerging suburban development is developing pursuant to a comprehensive specific plan (Specific Plan No. 152) that both accommodates potential population growth and provides for conservation of open space.

Lakeland Village

The community of Lakeland Village is located immediately west of Lake Elsinore and includes a major ridge along the eastern face of the Santa Ana and Elsinore Mountains. This community falls within the Lakeland Village Policy Area, which is comprised of a mix of rural, residential, light industrial, open space and commercial uses along Grand Avenue on the low lying areas near the lake. Natural open space with pockets of rural residential uses are adjacent to [State Route Highway 74](#) as it winds along the steep easterly face of the Santa Ana Mountains.



A "sphere of influence" is the area outside of and adjacent to a city's border that has been identified by the County Local Agency Formation Commission as a future logical extension of its jurisdiction. While the County of Riverside has land use authority over city sphere areas, development in these areas directly affects circulation, service provision, and community character within the cities.

Incorporated Cities

City of Lake Elsinore

The Elsinore Area Plan surrounds the incorporated City of Lake Elsinore. As of, the City of Lake Elsinore encompassed about 42.3 square miles, with an estimated population of 50,267, and 16,207 households. Lake Elsinore's sphere of influence encompasses over 30.2 square miles and extends into the Horsethief Canyon, Warm Springs and Meadowbrook communities and southwest towards the communities of El Cariso and Rancho Capistrano near the Main Divide Road.

City of Riverside

A portion of the City of Riverside's sphere of influence extends into the Warm Springs community. The City of Riverside's predominantly rural land use designations for this area are consistent with this area plan's direction.

City of Wildomar

Wildomar is located immediately south of the City of Lake Elsinore in a valley between the Santa Ana Mountains and the Gavilan and Sedco Hills. Wildomar City, incorporated on July 1, 2008, includes rural residential uses in the rolling hills and more intense concentration of residential, commercial and employment uses between Interstate 15 and Grand Avenue. The community is expanding easterly of Interstate 15, especially along Clinton Keith Road and Bundy Canyon Road.

City of Canyon Lake

Canyon Lake is a private, gated city located halfway between Lake Elsinore and Sun City, California. Canyon Lake began as a master-planned community developed by Corona Land Company in 1968. The "City of Canyon Lake" was incorporated on December 1, 1990. As of 2009, the city geographically spanned over 4.6 square miles. Originally formed in 1927 after Railroad Canyon Dam was built, the lake covers 383 acres and includes 14.9 miles of shoreline.

Land Use Plan

The Land Use Plan focuses on preserving the numerous unique features in the Elsinore area and, at the same time, guides the accommodation of future growth. To accomplish this, more detailed land use designations are applied than for the Countywide General Plan. Proposed uses represent a full spectrum of categories that relate the natural characteristics of the land and economic potential to a range of permitted uses.

The Elsinore Land Use Plan, Figure 3 depicts the geographic distribution of land uses within this area. The Plan is organized around 21 Area Plan land use designations. These land uses derive from, and provide more detailed direction than, the five General Plan Foundation Component land uses: Open Space, Agriculture, Rural, Rural Community and Community Development. Table 1, Land Use Designations Summary, outlines the development intensity, density, typical allowable land uses, and general characteristics for each of the area plan land use designations within each Foundation Component. The General Plan Land Use Element contains more detailed descriptions and policies for the Foundation Components and each of the area plan land use designations.

Many factors led to the designation of land use patterns. Among the most influential were the Riverside County Vision and Planning Principles, both of which focused, in part, on preferred patterns of development within the County of Riverside; the Community Environmental Transportation Acceptability Process (CETAP) that focused on major transportation corridors; the Multiple Species Habitat Conservation Plan (MSHCP) that focused on opportunities and strategies for significant open space and habitat preservation; established patterns of existing uses and parcel configurations; current zoning; and the oral and written testimony of Riverside County residents, property owners, and representatives of cities and organizations at the many Planning Commission and Board of Supervisors hearings. The result of these considerations is shown in Figure 3, Land Use Plan, which portrays the location and extent of proposed land uses. Table 2, Statistical Summary of the Elsinore Area Plan, provides a summary of the projected development capacity of the plan if all uses are built as proposed. This table includes dwelling unit, population, and employment capacities.

Land Use Concept

The Elsinore Area Plan reflects the RCIP Vision for Riverside County in several ways. It does so by intensifying and mixing uses at nodes adjacent to transportation corridors, by more accurately reflecting topography and natural resources in land use designations, by avoiding high intensity development in natural hazard areas, and by considering compatibility with adjacent communities' land use plans as well as the desires of residents in the plan area.

The land use designations maintain the predominantly very low density character of the Meadowbrook and Warm Springs communities, the natural and recreational characteristics of the Cleveland National Forest, and Community Development uses in Lakeland Village. Areas designated Conservation-Habitat and Rural Mountainous help provide a separation between communities and provide additional definition for existing communities.

“

Communities should range in location and type from urban to suburban to rural, and in intensity from dense urban centers to small cities and towns to rural country villages to ranches and farms.

”

- RCIP General Plan Principles

“

Our communities - both improvements to existing ones and newly emerging ones - are models for new ways to provide and manage infrastructure, deliver education, access jobs, apply new technology, and achieve greater efficiency in the use of land, structure, and public improvements.

”

- RCIP Vision

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Elsinore Area Plan

Figure 1: Elsinore Area Plan Location

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Figure 2: Elsinore Area Plan Physical Features

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Figure 3: Elsinore Area Plan Land Use Plan

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Table 1: Land Use Designations Summary

Foundation Component	Area Plan Land Use Designation	Building Intensity Range (du/ac or FAR) ^{1, 2, 3, 4}	Notes
Agriculture	Agriculture (AG)	10 ac min.	<ul style="list-style-type: none"> Agricultural land including row crops, groves, nurseries, dairies, poultry farms, processing plants, and other related uses. One single-family residence allowed per 10 acres except as otherwise specified by a policy or an overlay.
Rural	Rural Residential (RR)	5 ac min.	<ul style="list-style-type: none"> Single-family residences with a minimum lot size of 5 acres. Allows limited animal keeping and agricultural uses, recreational uses, compatible resource development (not including the commercial extraction of mineral resources) and associated uses and governmental uses.
	Rural Mountainous (RM)	10 ac min.	<ul style="list-style-type: none"> Single-family residential uses with a minimum lot size of 10 acres. Areas of at least 10 acres where a minimum of 70% of the area has slopes of 25% or greater. Allows limited animal keeping, agriculture, recreational uses, compatible resource development (which may include the commercial extraction of mineral resources with approval of a SMP) and associated uses and governmental uses.
	Rural Desert (RD)	10 ac min.	<ul style="list-style-type: none"> Single-family residential uses with a minimum lot size of 10 acres. Allows limited animal keeping, agriculture, recreational, renewable energy uses including solar, geothermal and wind energy uses, as well as associated uses required to develop and operate these renewable energy sources, compatible resource development (which may include the commercial extraction of mineral resources with approval of SMP), and governmental and utility uses.
Rural Community	Estate Density Residential (RC-EDR)	2 ac min.	<ul style="list-style-type: none"> Single-family detached residences on large parcels of 2 to 5 acres. Limited agriculture, intensive equestrian and animal keeping uses are expected and encouraged.
	Very Low Density Residential (RC-VLDR)	1 ac min.	<ul style="list-style-type: none"> Single-family detached residences on large parcels of 1 to 2 acres. Limited agriculture, intensive equestrian and animal keeping uses are expected and encouraged.
	Low Density Residential (RC-LDR)	0.5 ac min.	<ul style="list-style-type: none"> Single-family detached residences on large parcels of 0.5 to 1 acre. Limited agriculture, intensive equestrian and animal keeping uses are expected and encouraged.
Open Space	Conservation (C)	N/A	<ul style="list-style-type: none"> The protection of open space for natural hazard protection, cultural preservation, and natural and scenic resource preservation. Existing agriculture is permitted.
	Conservation Habitat (CH)	N/A	<ul style="list-style-type: none"> Applies to public and private lands conserved and managed in accordance with adopted Multi Species Habitat and other Conservation Plans and in accordance with related Riverside County policies..
	Water (W)	N/A	<ul style="list-style-type: none"> Includes bodies of water and natural or artificial drainage corridors. Extraction of mineral resources subject to SMP may be permissible provided that flooding hazards are addressed and long term habitat and riparian values are maintained.
	Recreation (R)	N/A	<ul style="list-style-type: none"> Recreational uses including parks, trails, athletic fields, and golf courses. Neighborhood parks are permitted within residential land uses.
	Rural (RUR)	20 ac min.	<ul style="list-style-type: none"> One single-family residence allowed per 20 acres. Extraction of mineral resources subject to SMP may be permissible provided that scenic resources and views are protected.
	Mineral Resources (MR)	N/A	<ul style="list-style-type: none"> Mineral extraction and processing facilities. Areas held in reserve for future mineral extraction and processing.

Table 1, continued

Foundation Component	Area Plan Land Use Designation	Building Intensity Range (du/ac or FAR) ^{1, 2, 3, 4}	Notes
Community Development	Estate Density Residential (EDR)	2 ac min.	<ul style="list-style-type: none"> Single-family detached residences on large parcels of 2 to 5 acres. Limited agriculture and animal keeping is permitted, however, intensive animal keeping is discouraged.
	Very Low Density Residential (VLDR)	1 ac min.	<ul style="list-style-type: none"> Single-family detached residences on large parcels of 1 to 2 acres. Limited agriculture and animal keeping is permitted, however, intensive animal keeping is discouraged.
	Low Density Residential (LDR)	0.5 ac min.	<ul style="list-style-type: none"> Single-family detached residences on large parcels of 0.5 to 1 acre. Limited agriculture and animal keeping is permitted, however, intensive animal keeping is discouraged.
	Medium Density Residential (MDR)	2 - 5 du/ac	<ul style="list-style-type: none"> Single-family detached and attached residences with a density range of 2 to 5 dwelling units per acre. Limited agriculture and animal keeping is permitted, however, intensive animal keeping is discouraged. Lot sizes range from 5,500 to 20,000 sq. ft., typical 7,200 sq. ft. lots allowed.
	Medium High Density Residential (MHDR)	5 - 8 du/ac	<ul style="list-style-type: none"> Single-family attached and detached residences with a density range of 5 to 8 dwelling units per acre. Lot sizes range from 4,000 to 6,500 sq. ft.
	High Density Residential (HDR)	8 - 14 du/ac	<ul style="list-style-type: none"> Single-family attached and detached residences, including townhouses, stacked flats, courtyard homes, patio homes, townhouses, and zero lot line homes.
	Very High Density Residential (VHDR)	14 - 20 du/ac	<ul style="list-style-type: none"> Single-family attached residences and multi-family dwellings.
	Highest Density Residential (HHDR)	14 - 40 du/ac	<ul style="list-style-type: none"> Multi-family dwellings, includes apartments and condominium. Multi-storied (3+) structures are allowed.
	Commercial Retail (CR)	0.20 - 0.35 FAR	<ul style="list-style-type: none"> Local and regional serving retail and service uses. The amount of land designated for Commercial Retail exceeds that amount anticipated to be necessary to serve Riverside County's population at build out. Once build out of Commercial Retail reaches the 40% level within any Area Plan, additional studies will be required before CR development beyond the 40 % will be permitted.
	Commercial Tourist (CT)	0.20 - 0.35 FAR	<ul style="list-style-type: none"> Tourist related commercial including hotels, golf courses, and recreation/amusement activities.
	Commercial Office (CO)	0.35 - 1.0 FAR	<ul style="list-style-type: none"> Variety of office related uses including financial, legal, insurance and other office services.
	Light Industrial (LI)	0.25 - 0.60 FAR	<ul style="list-style-type: none"> Industrial and related uses including warehousing/distribution, assembly and light manufacturing, repair facilities, and supporting retail uses
	Heavy Industrial (HI)	0.15 - 0.50 FAR	<ul style="list-style-type: none"> More intense industrial activities that generate greater effects such as excessive noise, dust, and other nuisances.
	Business Park (BP)	0.25 - 0.60 FAR	<ul style="list-style-type: none"> Employee intensive uses, including research and development, technology centers, corporate offices, clean industry and supporting retail uses.
	Public Facilities (PF)	< 0.60 FAR	<ul style="list-style-type: none"> Civic uses such as County of Riverside administrative buildings and schools.
Community Center (CC)	5 - 40 du/ac 0.10 - 0.3 FAR	<ul style="list-style-type: none"> Includes combination of small-lot single family residences, multi-family residences, commercial retail, office, business park uses, civic uses, transit facilities, and recreational open space within a unified planned development area. This also includes Community Centers in adopted specific plans. 	
Mixed-Use Area		<ul style="list-style-type: none"> This designation is applied to areas outside of Community Centers. The intent of the designation is not to identify a particular mixture or intensity of land uses, but to designate areas where a mixture of residential, commercial, office, entertainment, educational, and/or recreational uses, or other uses is planned. 	

Elsinore Area Plan

Table 1, continued

Overlays and Policy Areas	
Overlays and Policy Areas are not considered a Foundation Component. Overlays and Policy Areas address local conditions and can be applied in any Foundation Component. The specific details and development characteristics of each Policy Area and Overlay are contained in the appropriate Area Plan.	
Community Development Overlay (CDO)	<ul style="list-style-type: none"> Allows Community Development land use designations to be applied through General Plan Amendments within specified areas within Rural, Rural Community, Agriculture, or Open Space Foundation Component areas. Specific policies related to each Community Development Overlay are contained in the appropriate Area Plan.
Community Center Overlay (CCO)	<ul style="list-style-type: none"> Allows for either a Community Center or the underlying designated land use to be developed.
Rural Village Overlay (RVO) and Rural Village Overlay Study Area (RVOSA)	<ul style="list-style-type: none"> The Rural Village Overlay allows a concentration of residential and local-serving commercial uses within areas of rural character. The Rural Village Overlay allows the uses and maximum densities/intensities of the Medium Density Residential and Medium High Density Residential and Commercial Retail land use designations. In some rural village areas, identified as Rural Village Overlay Study Areas, the final boundaries will be determined at a later date during the consistency zoning program. (The consistency zoning program is the process of bringing current zoning into consistency with the adopted general plan.)
Historic District Overlay (HDO)	<ul style="list-style-type: none"> This overlay allows for specific protections, land uses, the application of the Historic Building Code, and consideration for contributing elements to the District.
Specific Community Development Designation Overlay	<ul style="list-style-type: none"> Permits flexibility in land uses designations to account for local conditions. Consult the applicable Area Plan text for details.
Policy Areas	<ul style="list-style-type: none"> Policy Areas are specific geographic districts that contain unique characteristics that merit detailed attention and focused policies. These policies may impact the underlying land use designations. At the Area Plan level, Policy Areas accommodate several locally specific designations, such as the Cherry Valley Policy Area (The Pass Area Plan), or the Highway 79 Policy Area (Sun City/Menifee Valley Area Plan). Consult the applicable Area Plan text for details.

NOTES:

1 FAR = Floor Area Ratio, which is the measurement of the amount of non-residential building square footage in relation to the size of the lot. Du/ac = dwelling units per acre, which is the measurement of the amount of residential units in a given acre.

2 The building intensity range noted is exclusive, that is the range noted provides a minimum and maximum building intensity.

3 Clustering is encouraged in all residential designations. The allowable density of a particular land use designation may be clustered in one portion of the site in smaller lots, as long as the ratio of dwelling units/area remains within the allowable density range associated with the designation. The rest of the site would then be preserved as open space or a use compatible with open space (e.g., agriculture, pasture or wildlife habitat). Within the Rural Foundation Component and Rural Designation of the Open Space Foundation Component, the allowable density may be clustered as long as no lot is smaller than 0.5 acre. This 0.5-acre minimum lot size also applies to the Rural Community Development Foundation Component. However, for sites adjacent to Community Development Foundation Component areas, 10,000 square foot minimum lots are allowed. The clustered areas would be a mix of 10,000-square-foot and 0.5-acre lots. In such cases, larger lots or open space would be required near the project boundary with Rural Community and Rural Foundation Component areas.

4 The minimum lot size required for each permanent structure with plumbing fixtures utilizing an onsite wastewater treatment system to handle its wastewater is 0.5 acre per structure.

HHDR was updated to 14 - 40 du/ac to be consistent with Housing Element 2021-2029 (09/28/21).

Elsinore Area Plan

Table 2: Statistical Summary of Elsinore Area Plan

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LAND USE	AREA		STATISTICAL CALCULATIONS ¹	
	ACREAGE ⁷	D.U.	POP.	EMPLOY.
LAND USE ASSUMPTIONS AND CALCULATIONS⁹				
LAND USE DESIGNATIONS BY FOUNDATION COMPONENTS				
AGRICULTURE FOUNDATION COMPONENT				
Agriculture (AG)	0	0	0	0
<i>Agriculture Foundation Sub-Total:</i>	<i>0</i>	<i>0</i>	<i>0</i>	<i>0</i>
RURAL FOUNDATION COMPONENT				
Rural Residential (RR)	2,441	366	1,106	NA
Rural Mountainous (RM)	10,414	521	1,592	NA
Rural Desert (RD)	0	0	0	NA
<i>Rural Foundation Sub-Total:</i>	<i>12,855</i>	<i>887</i>	<i>2,698</i>	<i>0</i>
RURAL COMMUNITY FOUNDATION COMPONENT				
Estate Density Residential (RC-EDR)	537	188	564	NA
Very Low Density Residential (RC-VLDR)	137	102	306	NA
Low Density Residential (RC-LDR)	36	54	162	NA
<i>Rural Community Foundation Sub-Total:</i>	<i>710</i>	<i>344</i>	<i>1,032</i>	<i>0</i>
OPEN SPACE FOUNDATION COMPONENT				
Open Space-Conservation (OS-C)	228	NA	NA	NA
Open Space-Conservation Habitat (OS-CH)	51,803	NA	NA	NA
Open Space-Water (OS-W)	334	NA	NA	NA
Open Space-Recreation (OS-R)	89	NA	NA	13
Open Space-Rural (OS-RUR)	6,496	162	489	NA
Open Space-Mineral Resources (OS-MIN)	0	NA	NA	0
<i>Open Space Foundation Sub-Total:</i>	<i>58,950</i>	<i>162</i>	<i>489</i>	<i>13</i>
COMMUNITY DEVELOPMENT FOUNDATION COMPONENT				
Estate Density Residential (EDR)	56	20	60	NA
Very Low Density Residential (VLDR)	3,200	2,400	7,250	NA
Low Density Residential (LDR)	454	681	2,057	NA
Medium Density Residential (MDR) ⁸	2,808	9,829	29,487	NA
Medium-High Density Residential (MHDR)	66	426	1,287	NA
High Density Residential (HDR)	11	119	359	NA
Very High Density Residential (VHDR)	17	288	870	NA
Highest Density Residential (HHDR)	0	0	0	NA
Commercial Retail ² (CR)	114	NA	NA	1,710
Commercial Tourist (CT)	17	NA	NA	282
Commercial Office (CO)	0	NA	NA	0
Light Industrial (LI)	632	NA	NA	8,215
Heavy Industrial (HI)	0	NA	NA	0
Business Park (BP)	34	NA	NA	552
Public Facilities (PF)	30	NA	NA	30
Community Center (CC) ³	0	0	0	0
Mixed-Use Area (MUA)	230	1,492	4,476	3,405
<i>Community Development Foundation Sub-Total:</i>	<i>7,669</i>	<i>15,260</i>	<i>45,780</i>	<i>14,194</i>
SUB-TOTAL FOR ALL FOUNDATION COMPONENTS:	80,184	15,422	49,999	14,207
NON-COUNTY JURISDICTION LAND USES				
OTHER LANDS NOT UNDER PRIMARY COUNTY JURISDICTION				
Cities	45,991	---	---	---
Indian Lands	0	---	---	---
Freeways	221	---	---	---
<i>Other Lands Sub-Total:</i>	<i>46,212</i>			
TOTAL FOR ALL LANDS:	126,396	15,422	49,999	14,207

Elsinore Area Plan

Table 2, continued

LAND USE	STATISTICAL CALCULATIONS ¹			
	AREA ACREAGE ⁷	D.U.	POP.	EMPLOY.
SUPPLEMENTAL LAND USE PLANNING AREAS				
<i>These SUPPLEMENTAL LAND USES are overlays, policy areas and other supplemental items that apply OVER and IN ADDITION to the base land use designations listed above. The acreage and statistical data below represent possible ALTERNATE land use or buildout scenarios.</i>				
OVERLAYS AND POLICY AREAS				
OVERLAYS^{4,5}				
Rural Village Study Area Overlay-	741	4,768	5,344	4,472
<i>Total Area Subject to Overlays:^{4,5}</i>	741	4,768	5,344	4,472
POLICY AREAS⁶				
Temescal Wash	444	---	---	---
Glen Eden	703	---	---	---
Warm Springs	13,834	---	---	---
Walker Canyon	1,248	---	---	---
Lakeland Village Policy Area	2,625	---	---	---
March Joint Air Reserve Base Influence Area	190	---	---	---
<i>Highway 74</i>				
<i>Total Area Within Policy Areas:⁶</i>	19,044			
TOTAL AREA WITHIN SUPPLEMENTALS:⁷	19,755			

FOOTNOTES:

- 1 Statistical calculations are based on the midpoint for the theoretical range of buildout projections. Reference Appendix E-1 of the General Plan for assumptions and methodology used.
- 2 For calculation purposes, it is assumed that CR designated lands will build out at 40% CR and 60% MDR.
- 3 Note that "Community Center" is used both to describe a land use designation *and* a type of overlay. These two terms are separate and distinct; are calculated separately; and, are *not* interchangeable terms.
- 4 ~~Overlays provide alternate land uses that may be developed instead of the underlying base use designations.~~
- 5 Policy Areas indicate where additional policies or criteria apply, *in addition* to the underlying base use designations. As Policy Areas are supplemental, it is possible for a given parcel of land to fall within one or more Policy Areas. It is also possible for a given Policy Area to span more than one Area Plan.
- 6 Overlay data represent the additional dwelling units, population and employment permissible under the alternate land uses.
- 7 A given parcel of land can fall within more than one Policy Area or Overlay. Thus, this total is *not* additive.
- 8 723.91 acres is under Glen Eden Policy Area which has an assumption of 2.5 du/ac.
- 9 Statistical calculation of the land use designations in the table represents addition of Overlays and Policy Areas.
- * Table was updated to include GPA Nos. 985, 988, 1122, 1156, 1166, 1223, and 1208; as well as city incorporations, adopted after DATE.
- * Table was updated to change Mixed-Use Planning Area to Mixed- Use Area, to be consistent with GPA No. 1122 Land Use Element

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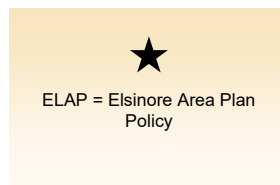
Overlays and Policy Areas

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A Policy Area is a portion of an area plan that contains special or unique characteristics that merit detailed attention and focused policies. The location and boundaries of the Policy Areas identified in the Elsinore Area Plan are shown on Figure 4, Overlays and Policy Areas, and are described in detail below.

Overlays and Policy Areas

Special policies are appropriate to address important locales that have special significance to the residents of this part of Riverside County. Six policy areas have been designated within the Elsinore Area Plan. Many of these policies derive from citizen involvement over a period of years in planning for the future of this area. In some ways, these policies are even more critical to the sustained character of the Elsinore area than some of the basic land use policies because they reflect deeply held beliefs about the kind of place this is and should remain. The policy area boundaries are only approximate and may be interpreted more precisely as decisions are called for in these areas. This flexibility, then, calls for considerable sensitivity in determining where conditions related to the policies actually exist, once a focused analysis is undertaken on a proposed development project.



Warm Springs

Located in the northern portion of the plan area, Warm Springs includes a rural area set within the steep slopes of the Gavilan Hills. The ridge line and slopes of the Gavilan Hills are biological and visual assets to the region.

Policies:

- ELAP 1.1 Protect the life and property of residents and maintain the character of the Gavilan Hills through adherence to the Hillside Development and Slope section of the General Plan Land Use Element, the Environmentally Sensitive Lands section of the Multipurpose Open Space Element, and the Slope and Soil Instability Hazards and Fire Hazards sections of the General Plan Safety Element.
- ELAP 1.2 Require that development of contiguous areas designated as Light Industrial be designed in a coordinated manner.
- ELAP 1.3 Require that all commercial and industrial uses be sensitive to environmental hazards (i.e., flooding) and not substantially impact environmental resources (i.e., biological and water quality).
- ELAP 1.4 Require commercial and industrial uses to not substantially impact circulation systems.

Temescal Wash

Temescal Wash, extending 28 miles from Lake Elsinore to the Santa Ana River, is the principal drainage course within the Temescal Valley. The Wash also serves as an important component of the Western Riverside County MSHCP and has the potential for providing recreational amenities to serve the planning area. The preservation

Elsinore Area Plan

and enhancement of this feature is an important component of the Elsinore Area Plan land use plan. This policy area is synonymous with the 100 year flood zone for the Wash.

Policies:

- ELAP 2.1 Protect the multipurpose open space attributes of the Temescal Wash through adherence to policies in the Flood and Inundation Hazards section of the General Plan Safety Element; the Non-motorized Transportation section of the Circulation Element; the Multiple Species Habitat Conservation Plans and the Environmentally Sensitive Lands sections of the Multipurpose Open Space Element; and the Open Space, Habitat and Natural Resource Preservation section of the Land Use Element.
- ELAP 2.2 Encourage the maintenance of Temescal Wash in its natural state, with its ultimate use for recreational and open space purposes such as trails, habitat preservation, and groundwater recharge.

Walker Canyon Policy Area

The Walker Canyon Policy Area consists of 1,250 acres of land located northerly of Interstate 15 in the vicinity of Walker Canyon Road. The site is designated Open Space-Rural on the Elsinore Area Plan. However, a preferable alternative to extremely large lot rural land sales would be the master planning of this area to provide for a limited amount of development, coupled with preservation of the majority of the site as open space and wildlife habitat.

Policies:

- ELAP 3.1 Notwithstanding the Open Space -Rural designation of this property, any proposal to establish a master planned community within this area through the general plan amendment and specific plan process shall be exempt from the eight-year limit and other procedural requirements applicable to Foundation Component amendments as described in the Administration Element, provided that:
- a. A specific plan is submitted for a Community Center or mixed use village center development designed as a hillside village. Potential uses may include residential uses at a variety of densities (including community development foundation component densities), commercial retail and service uses, offices, and a hotel, as well as public facilities and recreational areas. In addition to the required components, the specific plan must address the unique requirements of hillside development, special hillside design guidelines, and the special nuances of integrating hillside development into the natural environment.
 - b. Approximately 900 acres, or at least two-thirds of the site area, is set aside as Open Space - Conservation Habitat for inclusion in the Western Riverside County Multiple -Species Habitat Conservation Plan reserve system.
 - c. The specific plan shall include special attention to the following concerns: (1) pedestrian circulation in a hillside context, including provision for ramps and paths as well as stairs in order to ensure full accessibility for all users; (2) provision for retail commercial uses so as to minimize the need for residents to travel outside the village for routine daily needs, such as groceries, banking, etc.; and (3) the buffering and protection of conserved open space, especially relating to the interface between riparian areas and development.

- d. Due to the unique character of this development, the area is hereby determined to be eligible for reductions in onsite street widths and an exemption from the prohibition on development on slopes over 25%. Such exemptions would be subject to official determination by the Board of Supervisors or its successor-in-interest at the time of its action on the specific plan.

The environmental impact report or other CEQA document prepared for any specific plan at this site shall address the site's access, soils, geology, hydrology, biology, and wildfire susceptibility in addition to issues of slope and topography.

- e. Any such amendment shall be deemed an Entitlement/Policy amendment and be subject to the procedural requirements applicable to that category of amendments.

Glen Eden Policy Area

The Glen Eden Policy Area consists of portions of Sections 17, 18, and 19 located southwesterly of Temescal Canyon Road and northerly, northeasterly, and westerly of the Horsethief Canyon community. Development within this Policy Area shall be subject to the following policies.

Policies:

- ELAP 4.1 Residential development shall comply with an average density of 2.5 dwelling units per acre. No individual project may have an overall density in excess of 2.5 dwelling units per acre, unless a permanent density transfer between two or more projects is approved by the County of Riverside, in which case the overall density of the projects together may not exceed 2.5 dwelling units per acre. The density of individual parcels or planning areas within a project may exceed 2.5 dwelling units per acre, as long as the overall project density does not exceed this level.
- ELAP 4.2 Clustering of dwelling units within an individual project is encouraged where such clustering would enable the conservation of open space in accordance with the Multipurpose Open Space Element.

Highway 74 Policy Area

The Highway 74 Policy Area (Hwy 74 PA) is generally located along a 6.8-mile corridor of Highway 74 between the City of Lake Elsinore and the City of Perris. The policy area encompasses approximately 2,216 acres of unincorporated lands within the Elsinore Area Plan (ELAP) and the Mead Valley Area Plan (MVAP). The ELAP segment of the Hwy 74 PA is about half the total area with approximately 1,143 acres. This area can be defined to include the community of Warms Springs and Meadowbrook; northernly of Cambern Ave in the City of Elsinore and southernly of Ethanac Road.

CalTrans relinquished control and maintenance of this segment of Highway 74 to the County of Riverside on June 28, 2017. This provided an opportunity for the County to reassess development opportunities along one of busiest corridors in western Riverside County. The area is relatively rural with large vacant lots, single family residential homes, and small businesses, such as, liquor stores. The corridor is surrounded by hilly terrain and large boulders and is prone to periodic flooding. This area contains a portion of the Meadowbrook Disadvantaged Unincorporated Community as identified by the Local Agency Formation Commission (LAFCO). As such pursuant to Senate Bill (SB) 244, the County analyzed service issues faced by the Meadowbrook Area DUC. The analysis is included in the General Plan Appendix P. It concludes that the area requires additional stormwater drainage infrastructure to reduce flooding.

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Elsinore Area Plan

The Hwy 74 PA policies and related land use plan were developed as a result of extensive community input and are designed to support the development of residential neighborhoods of varying densities, neighborhood servicing commercial uses, and local employment center areas clustered along the Highway 74 corridor. The intent of the policy area is to stimulate economic development, provide housing opportunities, facilitate the development of infrastructure, and address Environmental Justice.

According to Figure LU-4B of the Land Use Element, the Hwy 74 PA is an Environmental Justice Community (EJC), which includes the communities of Good Hope, Meadowbrook, and Warm Springs. Therefore, areas within the Hwy 74 PA are subject to all relevant EJC policies of the Healthy Communities Element, which addresses civic engagement, reduction to health risks, and prioritization of infrastructure improvements. In furthering the intent of EJC, the Hwy 74 PA contains additional policies that address Environmental Justice concerns that are specific to this area.

Highway 74 Policy Area General Policies:

- ELAP 5.1 Encourage consolidation of parcels to promote better land use development and project design.
- ELAP 5.2 Where feasible the development of frontage/service roads should be encouraged to increase and facilitate access from Highway 74 to residential, commercial, and industrial sites
- ELAP 5.3 The Mixed-Use Area (MUA) Land Use Designation may be found consistent with any nonresidential zoning classification that implements the intent of the land use designation or provides for a community serving use(s).
- ELAP 5.4 Development should be coordinated with Riverside Transit Agency (RTA) to ensure bus routes are identified and bus stops are provided to adequately serve community residents.
- ELAP 5.5 Development may include live-work spaces within the MUAs where appropriate.
- ELAP 5.6 Development should promote a reduction of vehicle miles traveled (VMT) and livable and resilient neighborhoods that provide housing, goods and services, open space, and multi-modal transportation options within proximity to each other.
- ELAP 5.7 Trees, signage, landscaping, street furniture, public art, and other aesthetic elements should be used to enhance appearance and provide neighborhood uniqueness.
- ELAP 5.8 Commercial Parking: should be screened/buffered from any public right-of-way with incorporation of landscaping, walls, berms with trees in support of the streetscape.
- ELAP 5.9 Developments should be encouraged to design and locate convenient pedestrian and bicycle connections, bus, or shuttle connections, that increase connections to adjacent and nearby communities and cities, businesses, parks and open space areas, and new transit access opportunities.
- ELAP 5.10 Work on reducing illegal dumping, including hazardous waste, and increase access to affordable composting and recycling facilities; encourage the appropriate permitting of waste sites and reclamation of cleanup sites.

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ELAP 5.11 Encourage the connection of municipal water and wastewater services to community residents and facilities to reduce reliance on septic systems in order to limit groundwater contamination.

Highway 74 Policy Area Neighborhoods

These neighborhoods are important locations because they'll established a sense of uniqueness and community that differentiate them from each other and adjacent cities. As a result, many of the policies for Hwy 74 PA have derived from local citizen input. Therefore, the purpose of organizing the Highway 74 PA into three distinct neighborhoods is to:

- Encourage stronger neighborhood character and sense of place; and,
- Reduced distances between housing, workplaces, retail businesses and other amenities and destinations; and,
- Facilitate the creation of walkable, bicycle-friendly environment with increased accessibility via public transit; and,
- Encourage revitalization of the area, by encourage new economic development, that promote new localized infrastructure improvements; and,
- Promote Environmental Justice appropriately.

The Highway 74 Policy Area contains a total of three neighborhoods. Neighborhoods 2 and 3 are located within the ELAP. Neighborhood 1 is located within the MVAP.

- Neighborhood 1: generally located north of Ethanac Road and south of 7th Street in the City of Perris; and within the Mead Valley Area Plan.
- Neighborhood 2: generally located north of Crumpton Street in the City of Elsinore and south of Ethanac Road; and within the Elsinore Area Plan.
- Neighborhood 3: generally located north of Cambern Avenue and south of Trellis Lane in the City of Elsinore; and within the Elsinore Area Plan.

Description of Neighborhood

Neighborhood – 2 primarily has single-story homes on large lots and establishments such as market shops and vehicle repair shops. This neighborhood has land use designations of Commercial Retail, Business Park, and Mixed-Use Areas, and has Very Low-Density Residential on the outskirts of its boundary.

This neighborhood presents opportunity to serve as an entry point from the City of Elsinore to the Highway 74 Policy Area, that provides a sense of uniqueness, and contains commercial and clean industry establishments, that support residential components that facilitate a “live, work, and play” environment.

Policies

ELAP 5.12 New developments within the neighborhood should support the neighborhood's emerging identity.

ELAP 5.13 Encourage complete streets, which include sidewalks, greenbelts, and trails to facilitate use by pedestrians and bicyclists where such facilities are well separated from parallel or

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cross through traffic to ensure pedestrian and cyclist safety.

ELAP 5.14 Work on preserving outstanding scenic vistas and features and encouraging underground placement of electric or communication distribution lines.

Neighborhood – 3 has industrial and commercial establishments and is mostly surrounded by the City of Elsinore. This neighborhood has land use designations of Commercial Retail, Business Park, Light Industrial and some Very Low-Density Residential on the outskirts of its boundary. This neighborhood presents the opportunity to provide local employment to residents.

Policy

ELAP 5.15 Encourage effective and comprehensive coordination efforts with the City of Lake Elsinore regarding planning programs, including circulation policies, that affect commercial and industrial development/entitlement activity.

Rural Village Land Use Overlay

~~Rural Village Overlay Study Areas were identified on the Elsinore Area Plan map for the community of Meadowbrook (along State Highway Route 74 northeasterly of the City of Lake Elsinore) in the 2003 General Plan. Prior to the adoption of the 2008 General Plan Update, all relevant factors were studied in more detail on a parcel-by-parcel basis through a spatial analysis. As a result of this analysis, county review, and community discussion, the boundary and policies of these study areas were modified and a Rural Village Land Use Overlay was created to strategically intensify the uses in the targeted core areas of Meadowbrook (Figure 5), but not in El Cariso.~~

~~The spatial analysis indicated that the increase in intensity of uses in El Cariso Rural Village is not necessary at this particular time, thus resulting in removing the boundaries of the Rural Village Study Area established in the RCPD General Plan.~~

Policies:

~~ELAP 5.1 Allow areas designated with the Rural Village Land Use Overlay to develop according to the standards of this section. Otherwise, the standards of the underlying land use designation shall apply.~~

~~ELAP 5.2 In the Meadowbrook Land Use Overlay, commercial uses, small-scale industrial uses (including mini-storage facilities), and residential uses at densities higher than those levels depicted on the Area Plan may be approved as designated in the overlay. Additionally, existing commercial and industrial uses may be relocated to this Rural Village Land Use Overlay as necessary in conjunction with the widening of State Highway Route 74.~~

Meadowbrook Town Center

~~Meadowbrook Town Center (see Figure 3A) features two areas of intense, Mixed-Use Area development clustering the Highway 74/Meadowbrook Avenue Neighborhood [Neighborhood 1] and the Highway 74/Kimes Lane Neighborhood [Neighborhood 2] to provide a broad panoply of conveniently located local community services.~~

and an expanded variety of housing opportunities for local residents. These Mixed Use Areas, described below, will provide landowners with opportunities to develop their properties for either all residential development (at varying urban densities) or a mixture of residential and nonresidential development. Those who choose to develop mixed uses on their properties will be able to utilize either side-by-side or vertically integrated land use designs. Both neighborhoods require that at least 50% of their areas be developed for Highest Density Residential (HHDR) uses.

Potential nonresidential uses include those traditionally found in a “downtown/Main Street” setting, such as retail uses, eating establishments, personal services such as barber shops, beauty shops, and dry cleaners, professional offices, and public facilities including schools, together with places of religious assembly and recreational, cultural, and spiritual community facilities, all integrated with small parks, plazas, and pathways or paseos. Together these designated Mixed Use Areas will provide a balanced mix of jobs, housing, and services within compact, walkable neighborhoods that feature pedestrian and bicycle linkages (walking paths, paseos, and trails) between residential uses and activity nodes such as grocery stores, pharmacies, places of assembly, schools, parks, and community and/or senior centers.

Mixed-Use Area (MUA) Neighborhoods Descriptions and Policies:

Following are the descriptions of the two Mixed Use Area (MUA) neighborhoods of Meadowbrook Town Center, and the policies specific to each neighborhood:

The Highway 74/Meadowbrook Avenue Neighborhood [Neighborhood 1] The Highway 74/Meadowbrook Avenue Neighborhood is bisected by State Highway 74. This neighborhood covers about 56 gross acres (about 39 net acres), and currently contains low density single family residences and vacant lots. The neighborhood is surrounded by similar land uses—low density single family residences and vacant parcels. The neighborhood will be developed as a Mixed Use Area, with a 50% HHDR component, and commercial and other land use types. Surrounding land uses are designated Very Low Density Residential.

Two bus stops are currently located on Highway 74 towards the northernmost boundary of the neighborhood, one located to serve northbound passengers, and one located to serve southbound passengers. Commercial and other types of non-residential mixed use development will be most appropriately placed directly along and near Highway 74, which is convenient for those living in and commuting into the neighborhood and will provide a buffer from the highway for the HHDR residential development in the neighborhood. Also, the opportunity exists to expand transit services and provide more bus stops and more bus services along Highway 74, as local transit demand expands in the future.

Also, because of its mixed use characteristics, this neighborhood should be designed to promote a village style mix of retail, restaurants, offices, and multi-family housing, thereby resulting in a walkable neighborhood. This neighborhood would serve surrounding neighborhoods by providing job opportunities through its commercial uses. It should be noted that this neighborhood is within a flood zone which could result in additional permits to meet floodplain management requirements, and would provide opportunities for open space buffers between differing use types, as needed, and opportunities for open space edge trails.

Policies:

ELAP 5.3 — Fifty percent of the Highway 74/Meadowbrook Avenue Neighborhood shall be developed in accordance with the HHDR land use designation.

ELAP 5.4 — Residential uses for the Highway 74/Meadowbrook Avenue Neighborhood should generally be located in the southeastern and northeastern portions of this neighborhood. Nonresidential uses

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~~should include a variety of other uses, such as retail activities serving the local population and tourists, parks, light industrial uses, parkland, and other uses.~~

~~**Highway 74/Kimes Lane Neighborhood [Neighborhood 2]** is located less than one mile north of Neighborhood 1 and also along State Highway 74, on about 10 gross acres (about 7 net acres). With the exception of one single family residence, the neighborhood site is currently vacant and is surrounded by low density single family residential uses and vacant parcels. Highway 74 adjoins the western edge of the neighborhood. This neighborhood will be developed as a Mixed Use Area, with a 50% HHDR component, and commercial and other land use types. This neighborhood is surrounded by Very Low Density Residential land uses.~~

~~This neighborhood could serve the surrounding community by providing local commercial services and job opportunities in association with the commercial uses. Also, because of its mixed use characteristics, this neighborhood would be designed to promote a village style mix of retail, restaurants, offices, and multi family housing, resulting in a walkable neighborhood. Two bus stops are conveniently located on Highway 74 within the neighborhood boundaries. It should be noted that this neighborhood is within a flood zone which could result in additional permits to meet the community's floodplain management requirements, and would provide opportunities for open space buffers between differing use types, as needed, and opportunities for open space edge trails.~~

~~Policies:~~

~~ELAP 5.5 — Fifty percent of the Highway 74/Kimes Lane Neighborhood shall be developed in accordance with the HHDR land use designation.~~

~~ELAP 5.6 — Residential uses for the Highway 74/Kimes Lane Neighborhood [Neighborhood 2] should be encouraged to be located in the eastern portion of this neighborhood. Nonresidential uses should include a variety of other uses, such as retail activities serving the local population and tourists, business parks, light industrial uses, and parkland.~~

~~Policies Applying to both Neighborhoods of Meadowbrook Town Center:~~

~~The following policies apply to both of the Mixed Use Area (MUA) neighborhoods of Meadowbrook Town Center:~~

~~ELAP 5.7 — Both the Highway 74/Meadowbrook Avenue and Highway 74/Kimes Lane Neighborhoods shall be developed with 50 % Highest Density Residential, and other uses, potentially including commercial, business park, office, etc. uses, in a mutually supportive, mixed use development pattern.~~

~~ELAP 5.8 — Paseos and pedestrian/bicycle connections should be provided between the Highest Density Residential uses and those nonresidential uses that would serve the local population. Connections should also be provided to the public facilities in the vicinity, including the elementary school, library, and community center.~~

~~ELAP 5.9 — All HHDR sites should be designed to facilitate convenient pedestrian, bicycle, and other non-motorized vehicle access to the community's schools, jobs, retail and office commercial uses, park and open space areas, trails, and other community amenities and land uses that support the community needs on a frequent and, in many cases, daily, basis.~~

~~ELAP 5.10 — Ensure that all new land uses, particularly residential, commercial, and public uses, including schools and parks, are designed to provide convenient public access to alternative transportation~~

~~facilities and services including potential future transit stations, transit oasis type shuttle systems, and/or local bus services, and local and regional trail systems.~~

~~ELAP 5.11 Project designs should reduce traffic noise levels from Highway 74 as perceived by noise sensitive uses, such as residential uses, to acceptable levels.~~

~~ELAP 5.12 Residential uses that are proposed in both neighborhoods where they would be located immediately adjacent to areas designated for Low Density Residential development should include edge sensitive development features to provide buffering between the differing residential densities, including but not necessarily limited to such features as one story buildings, park lands and open space areas, and trails.~~

~~ELAP 5.13 Uses approved and operating under an existing valid entitlement may remain or be converted into another land use in accordance with Riverside County Ordinance No. 348 and consistent with these policies.~~

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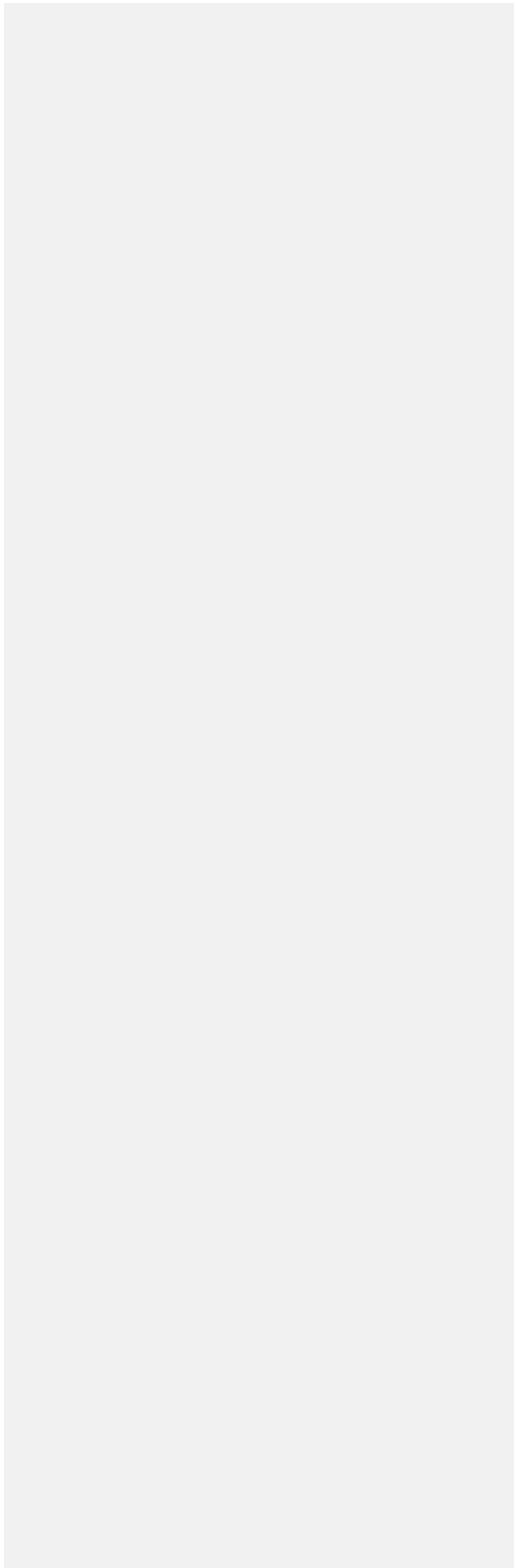
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Figure 3A: ~~Elsinore Area Plan Meadowbrook Town Center Neighborhoods~~ Highway 74 Plan Area Neighborhoods

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Figure 3B: Elsinore Area Plan Lee Lake Community Neighborhoods



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Lakeland Village Policy Area

The Lakeland Village Policy Area (LVPA) is located on the westerly side of the water body that is Lake Elsinore and is nestled against the easterly side of Cleveland Ridge along the eastern flank of the Santa Ana and Elsinore Mountains. The Lakeland Village Policy Area consists of approximately 2,626 acres, which includes a large portion of the unincorporated community of Lakeland Village, generally bounded by [State Route Highway 74](#), or the Ortega Highway, and the City of Lake Elsinore limits on the northerly end and Corydon Road and the City of Wildomar on the southerly end. Grand Avenue runs the length of the community and is the only roadway access to the area from the north and the south. Existing uses in the community are primarily single-family residential with pockets of commercial uses scattered along Grand Avenue. Properties east of Grand Avenue generally extend to the edge of the lake, which may be part of a Special Flood Hazard Area due to the significant water level fluctuations of Lake Elsinore. Properties on the westerly side of Grand Avenue extend up to the base of the hills and may include areas with steep slopes.

Policies:

- ELAP 6.1 Land within the Special Flood Hazard Areas should be developed in accordance with all applicable local, state and federal flood control ordinances and regulations, including the *Lake Village Master Drainage Plan*, and may include passive recreational uses.
- ELAP 6.2 In addition to Specific Plan and Mixed-Use zoning classifications, commercial zoning classifications that implements the intent of the land use designation or provide for a community serving use(s) may be utilized for any Mixed-Use Area (MUA) General Land Use Designation within the Lakeland Village Policy Area (LVPA).
- ELAP 6.3 Encourage the design of new streets and the significant upgrading of existing streets to provide all users with safe, convenient access through the community. Emphasis should be placed on providing dedicated, protected facilities for pedestrians and bicyclists, including a continuous network of sidewalks and pedestrian pathways; bicycle routes and lanes; multi-use trails and trailhead parking; traffic calming measures; and delineated street crossings where feasible.
- ELAP 6.4 Encourage the formation of a County Service Area (CSA) or Parks and Recreation District to develop adequate park services and facilities. Large-scale development is encouraged to include parks, recreational open space, plazas and other public spaces.
- ELAP 6.5 Development should provide for continuous Collector roadways, especially along Union and Brightman Avenues between Blanchie Drive and Turner Street, in order to provide for parallel travel with Grand Avenue and should provide for street connections to Grand Avenue via Blanchie Drive and Turner Street, which should also be developed as Collector roadways.
- ELAP 6.6 Encourage the clustering of development and consolidation of parcels, whenever feasible. (AI 25, AI 59-61)
- ELAP 6.7 Development of parcels not designated Rural Mountainous with steep slopes should cluster buildings in areas with lesser slope and should comply with hillside design policy in the Land Use Element. Residential densities of any parcel with slopes greater than 35 percent should be one (1) dwelling unit per twenty (20) acres.
- ELAP 6.8 Building envelopes and locations should be visually compatible with the surrounding uses.

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ELAP 6.9 The community's history and character should be incorporated into all streetscapes and development.

Lakeland Village Policy Area Neighborhoods

The Lakeland Village Policy Area includes eight neighborhoods, known as "LVPA Neighborhood," located along Grand Avenue, seven of which have been designated, partly or in whole, the General Plan Land Use Designation of Light Industrial that will remain. The LVPA Neighborhoods include mixed use and other complimentary land uses that encourage a combination of business, office, retail, commercial use, community facilities and residential uses that are physically and functionally integrated. The intent of the LVPA Neighborhoods is not to designate areas where a blend of uses can be developed. Mixed use development provides the following community benefits:

- Greater housing variety and density, more affordable housing, life-cycle housing (e.g. starter homes to larger family homes to senior housing), workforce housing, veterans housing, etc.;
- Reduced distances between housing, workplaces, retail businesses and other amenities and destinations;
- Better access to fresh, healthy foods (as food and retail and farmers markets can be accessed on foot/bike or by transit);
- More compact development, land use synergy (e.g. residents provide customers for retail which provide amenities for residents);
- Stronger neighborhood character and sense of place;
- Walkable, bicycle-friendly environments with increased accessibility via transit resulting in reduced transportation costs;
- Encourage the assembly of small parcels into larger project areas that can be developed for mixed residential/commercial development without the requirement for general plan amendments, helping to revitalize the area, encourage new balanced economic development, and provide for new local infrastructure improvements; and,
- Encourage commercial development to be near intersections and focused in nodes or village focus areas, as opposed to strip or piecemeal development spread along the Grand Avenue corridor.

In addition to the policies provided above, specific policies related to development within the LVPA Neighborhoods are described below:

Lakeland Village Policy Area Neighborhood Policies

The following policies apply to all Neighborhoods in the Lakeland Village Policy Area, unless specified differently within any policy.

Policies:

ELAP 6.10 New development in MUAs are encouraged to vary in residential densities, which may include ranges from 2 to 20 dwelling units per acre, and provide diversity in land uses.

ELAP 6.11 The density of residential development should complement the adjacent existing uses, generally transitioning from higher densities closer to Grand Avenue and commercial use development, to lower densities around the Mixed Use Area's edges that correspond with the residential densities located in the surrounding areas.

- ELAP 6.12 Areas with a MUA land use designation are intended to allow a mixture of compatible land uses including residential, administrative and professional offices, retail and service uses, public and quasi-public uses, and entertainment and recreational.
- ELAP 6.13 New development within Neighborhoods should promote livable neighborhoods that provide housing, goods and services, open space, and multi-modal transportation options within close proximity.
- ELAP 6.14 New non-residential development in the Neighborhoods 1 and 8 is encouraged to include uses that serve the needs of visitors and travelers, as well as residents of the area. Development in these neighborhoods should be designed to create a sense of arrival to Lakeland Village.
- ELAP 6.15 New non-residential development in the Neighborhoods 2 through 7 is encouraged to include uses that primarily serve the needs of residents living near the site or elsewhere in the community.
- ELAP 6.16 Neighborhoods are encouraged to include uses that serve the recreational needs of residents and visitors with such activities as hiking, mountain biking, boating, water sports, paragliding, skydiving, and other recreational uses due to the proximity of natural resources.
- ELAP 6.17 Development may include live-work spaces within the MUAs where appropriate.
- ELAP 6.18 New development within Neighborhood should be compatible with adjacent uses.
- ELAP 6.19 New development within Neighborhoods are encouraged to utilize distinctive architecture, edge and entry treatment, landscape, streetscaping, signage and other elements to perpetuate or establish a unique identity of the area.
- ELAP 6.20 Commercial uses, where applicable, should be oriented towards Grand Avenue and away from residential areas located outside of the Neighborhood, as feasible. Residential uses, where feasible and appropriate, should be used as a transitional buffer between the nonresidential and mixed uses within the Neighborhood and the lower density residential uses beyond.
- ELAP 6.21 Multi-story buildings are encouraged within commercial and mixed use areas with transitions down to two- or one-story buildings adjacent to residential neighborhoods, as appropriate.
- ELAP 6.22 Encourage the incorporation of variety of different types of wall textures and colors, architectural elements, landscaping and other features that provide for attractive and inviting facades for public view from surrounding uses and streets.
- ELAP 6.23 Ground floor commercial and facades are encouraged on the first floor of buildings facing the adjoining sidewalks and pedestrian spaces.
- ELAP 6.24 Encourage screening of off-street parking by locating it safely behind or within structures, or otherwise screening it from the public right-of-way, and the design of parking facilities with limited vehicle access points to optimize pedestrian safety, where feasible.
- ELAP 6.25 Street trees, signage, landscaping, street furniture, public art, and other aesthetic elements should be used to enhance the appearance and identity of the Neighborhoods.

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- ELAP 6.26 Encourage the use or installation of underground utilities.
- ELAP 6.27 Encourage coordination with local transit authorities to expand transit access along Grand Avenue and provide stops at, or close in proximity to each Neighborhood.
- ELAP 6.28 At least ten percent of the gross area of each Neighborhood should be reserved for common, integrated open space that provides opportunities for passive and active recreation.

Descriptions of LVPA Neighborhoods

Below are descriptions of each of the eight LVPA Neighborhoods, which may include neighborhood-specific policies, which only applies to that neighborhood.

Neighborhood 1

Neighborhood 1 is located and adjacent to the southwest side of Grand Avenue, generally northwest of Magnolia Street and southeast of the City of Lake Elsinore boundary, and consists of approximately 74 acres, as shown on Figure 3C, Elsinore Area Plan Lakeland Village Neighborhood 1. This neighborhood is predominately designated Mixed-Use Areas but includes some High Density Residential (HDR) and Very High Density Residential (VHDR) land use designations.

Neighborhood 1 is largely vacant with some existing commercial establishments on the northwestern end, abutting Grand Avenue, and a community center, which may be considered the focal point of this developing neighborhood due to its prominence in the area. Additionally, the neighborhood includes two existing multi-family residential complexes, located adjacent to the community center. There are three existing bus stops along Grand Avenue adjacent or in close proximity to this neighborhood.

This neighborhood presents opportunity for visitor- or commuter-serving commercial establishments, civic and community facilities, and supporting residential components that may provide a live, work, and play space that promotes active transportation, which includes use of transit from one of the nearby bus stops. Neighborhood 1 is shown on Figure 3C.

Policy

- ELAP 6.29 New development within Neighborhood 1 should cluster public, commercial, and residential uses that support this neighborhood's emerging identity as the civic center in the community.

Neighborhood 2

Neighborhood 2 abuts and is located southwest of Grand Avenue, generally northwest of Adelfa Street and southeast of Evergreen Street, and includes approximately 32 acres, as shown on Figure 3D, Elsinore Area Plan Lakeland Village Neighborhoods 2 & 3. This neighborhood is entirely designated as Mixed-Use Area.

This neighborhood is predominantly vacant with a small existing commercial center and one existing residential home in the center and southeastern portion. Neighborhood 2 includes a vast amount of large, contiguous vacant parcels of land covering most of this neighborhood.

This neighborhood presents an attractive opportunity for new development and would be a great opportunity for a well-balanced vertical or horizontal mix use area, with a diverse blend of commercial and residential uses clustered together. Such uses should include community-serving uses that serve this neighborhood's residents, as well as the Lakeland Village community, and recreation-serving uses that meet the recreational needs of visitors that come to Lakeland Village to enjoy its natural assets. In order to balance this area, residential uses are encouraged to include higher-density residential development and "Live-Work" units, which reduces the vehicle miles travelled within the community, amongst a wide variety of residential products. Neighborhood 2 is shown on Figure 3D.

Neighborhood 3

Neighborhood 3 abuts and is located southwest of Grand Avenue, north of Blackwell Boulevard and south of Deeble Entrance Street, and includes 24 acres, as shown on Figure 3D, Elsinore Area Plan Lakeland Village Neighborhoods 2 & 3. The neighborhood is predominantly a Mixed-Use Area land use designation, with a limited area of Commercial Retail (CR) in-between the neighborhood.

Neighborhood 3 is largely vacant, with Riverside County Fire Department Station 11 located along Grand Avenue in between Maiden Lane and Lillian Ave, as well as a residence located adjacent to the fire station. Neighborhood 3 is characterized by multiple large, vacant parcels in the northern portion of the neighborhood, with smaller parcels to the south.

Thus, this neighborhood presents an opportunity for vertical or horizontal mixed use development, particularly on the larger vacant parcels. This neighborhood should foster a diverse mix of commercial and residential uses that can serve the neighborhood as well as the community. In order to balance this area, residential uses are encouraged to include higher-density residential development and "Live-Work" units, which reduces the vehicle miles travelled within the community, amongst a wide variety of residential products. Neighborhood 3 is shown on Figure 3D.

Neighborhood 4

Neighborhood 4 is located southwest of Grand Avenue, generally north of Vail Street and south of Turner Street, and consists of approximately 23 acres, as shown on Figure 3E, Elsinore Area Plan Lakeland Village Neighborhoods 4 & 5. This neighborhood is entirely designated as Light Industrial.

This neighborhood contains a mix of existing non-residential uses, predominantly industrial establishments with limited commercial facilities. The Neighborhood contains a number of larger lots, as well as many parcels that currently have a limited lot coverage.

This neighborhood presents a unique opportunity to allow for the continuance of existing industrial uses, while a providing long-range goal of converting into a mixed-use area that would mirror Neighborhood 5. Neighborhood 4 is shown on Figure 3E.

Policy

ELAP 6.30 Legally existing industrial uses may remain in accordance with Ordinance No. 348 and applicable approved land use permits with no further extensions to the life of the permit. Unpermitted and new industrial uses will need to go through the appropriate land use review process including placing a life on the land use permit for no longer than five (5) years or until the Neighborhood's General Plan Land Use designation is changed to MUA, whichever comes last, in order to meet the long-range mixed use intent of all LVPA Neighborhoods.

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Neighborhood 5

Neighborhood 5 abuts and is located southwest of Grand Avenue, generally north of Ginger Lane and South of Kathryn Way, and includes approximately 13 acres, as shown on Figure 3E, Elsinore Area Plan Lakeland Village Neighborhoods 4 & 5. This Neighborhood is entirely designated a Mixed-Use Area.

This neighborhood is predominantly vacant, with minimal existing residential homes, as well as a limited number of industrial and commercial facilities. Neighborhood 5 includes a large amounts of vacant land, and is dominated by large parcels with minimal existing lot coverage.

This neighborhood presents an opportunity to establish a commercial center in this part of the policy area. The surrounding residences, as well as the industrial uses to the north, present opportunities for supporting uses as well as neighborhood serving uses. The commercial center should include uses that benefit and serve this neighborhood's residents, as well as the overall Lakeland Village community. Neighborhood 5 is shown on Figure 3E.

Neighborhood 6

Neighborhood 6 abuts and is located southwest of Grand Avenue generally north of Zinck Way and south of Pamela Road, and consists of approximately 16 acres, as shown on Figure 3F, Elsinore Area Plan Lakeland Village Neighborhoods 6 & 7. The neighborhood designated as Mixed-Use Area.

This neighborhood includes a number of existing single-family residential homes, with large parcels in the northern portion of the neighborhood. The neighborhood is generally underdeveloped, with large areas of vacant land, abutting the hillsides to the southwest. The neighborhood is across Grand Avenue from the Lakeland Village Middle School, and surrounded by other residential uses in all directions.

This neighborhood is prime for development and presents great opportunity for a well-balanced vertical or horizontal mix use area, with a diverse blend of commercial and residential uses clustered together. Such uses should include community-serving uses that serve this neighborhood's residents, students and faculty of the adjacent school, as well as the surrounding residential developments. Neighborhood 6 is shown on Figure 3F.

Neighborhood 7

Neighborhood 7 abuts and is located northeast of Grand Avenue, generally north of Stoneman Street and south of Morrison Plane, and consists of approximately 7 acres, as shown on Figure 3F, Elsinore Area Plan Lakeland Village Neighborhoods 6 & 7. The neighborhood is designated entirely Mixed-Use Area.

This neighborhood is vacant and is made up of four larger parcels. The neighborhood is surrounded by residential development, and is in close proximity to the Lakeland Village Middle School, as well as Neighborhood 6.

This neighborhood presents an opportunity for residential development, potentially with a higher density than the surrounding uses. This neighborhood could also include a blend of commercial and residential uses clustered together that serve this neighborhood's, students and faculty of the adjacent school, as well as the surrounding residential developments. Neighborhood 7 is shown on Figure 3F.

Neighborhood 8

Neighborhood 8 abuts and is located northeast of Grand Avenue, generally north of Corydon Street and south of Gill Lane, and consists of approximately 19 acres, as shown on Figure 3G, Elsinore Area Plan Lakeland Village Neighborhood 8. This neighborhood is predominantly a Mixed-Use Area with a Commercial Retail (CR) area located at the intersection of Corydon Road Grand Avenue.

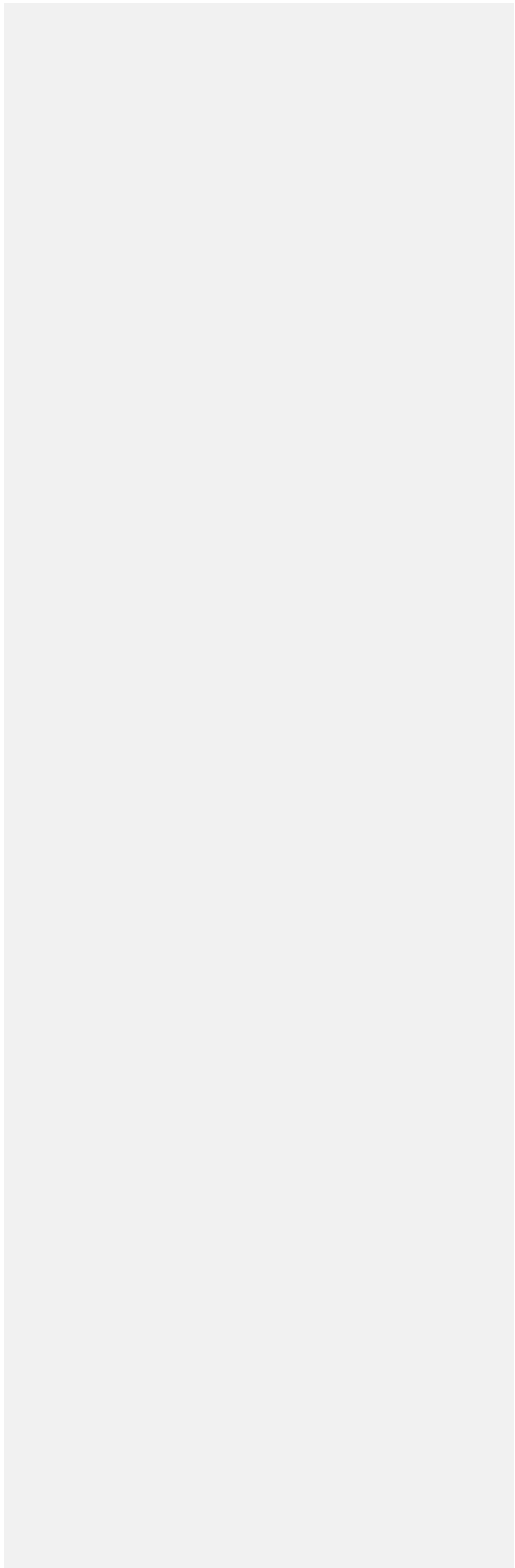
This neighborhood is predominantly vacant, with existing development generally confined to the southeast corner of the neighborhood. Existing development includes an existing commercial center, as well as single family residences located in the southwest portion of the site, adjacent to the commercial center, and along Gill Lane. The neighborhood contains a number of larger parcels that are vacant.

This neighborhood is a key local resource for residents who visit the existing commercial use. This neighborhood presents opportunity for visitor- or commuter-serving commercial establishments, and supporting residential components that may provide a live, work, and play space. Some of the community services that would benefit the neighborhood include additional retail, eating establishments, professional offices, dry cleaners, and a beauty salon that would meet the need of various residents in this neighborhood. Neighborhood 8 is shown on Figure 3G.

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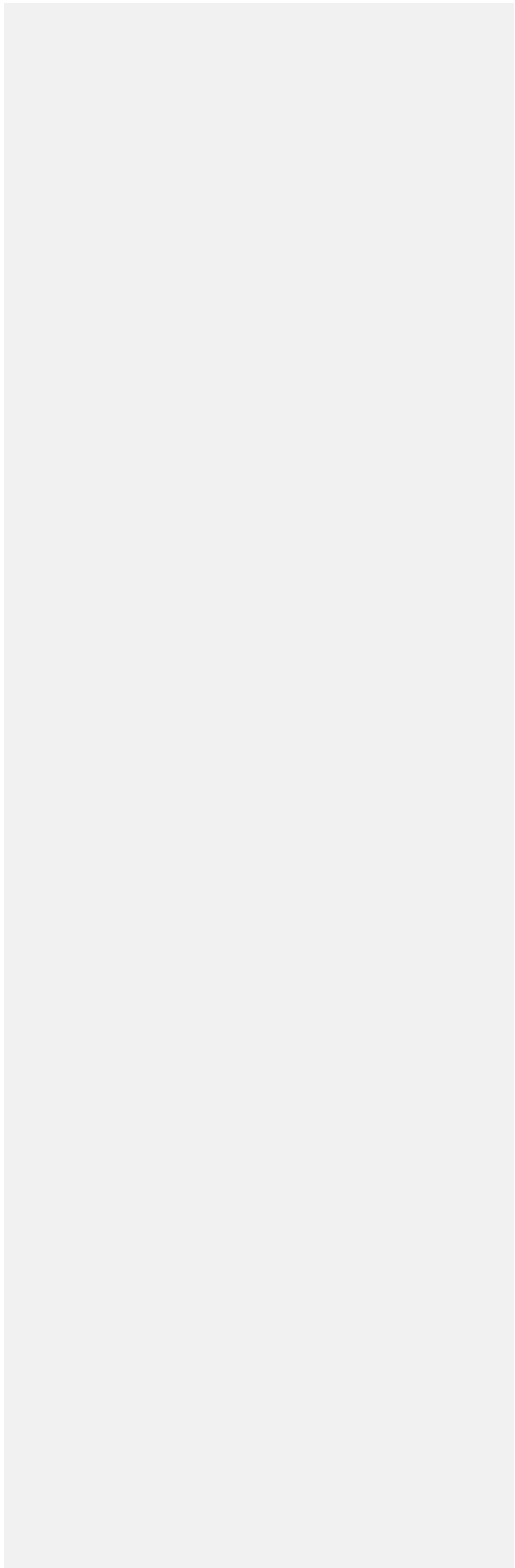
Figure 3C: Lakeland Village Neighborhood 1



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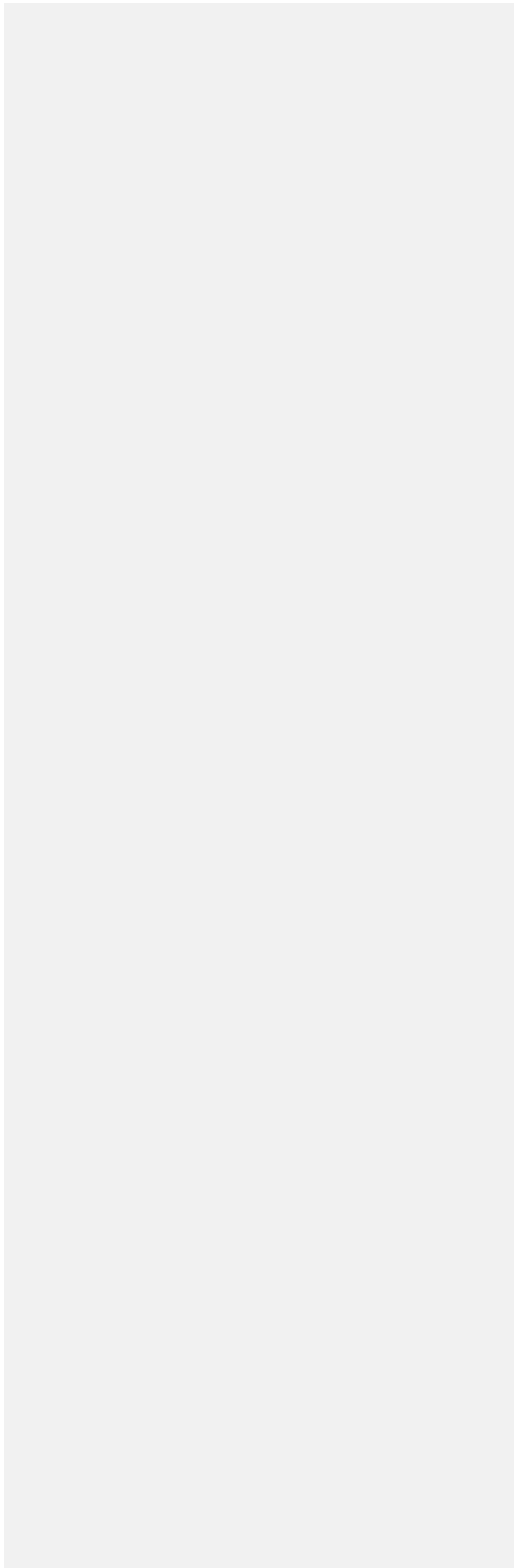
Figure 3D: Lakeland Village Neighborhoods 2 and 3



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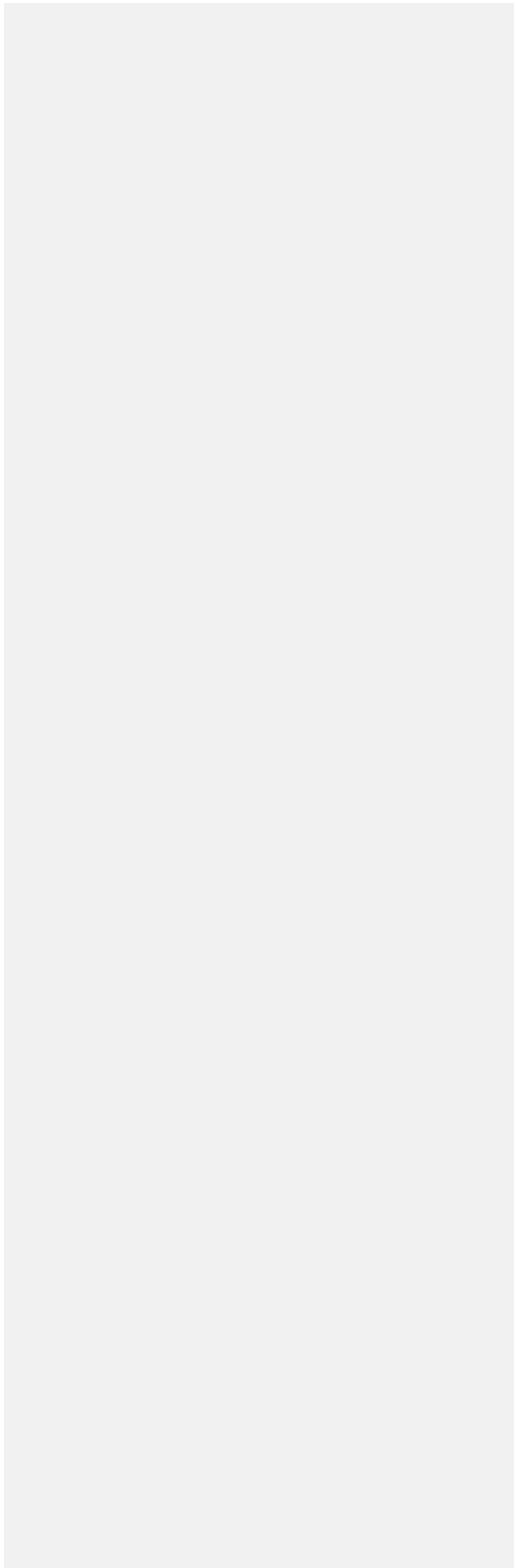
Figure 3E: Lakeland Village Neighborhoods 4 and 5



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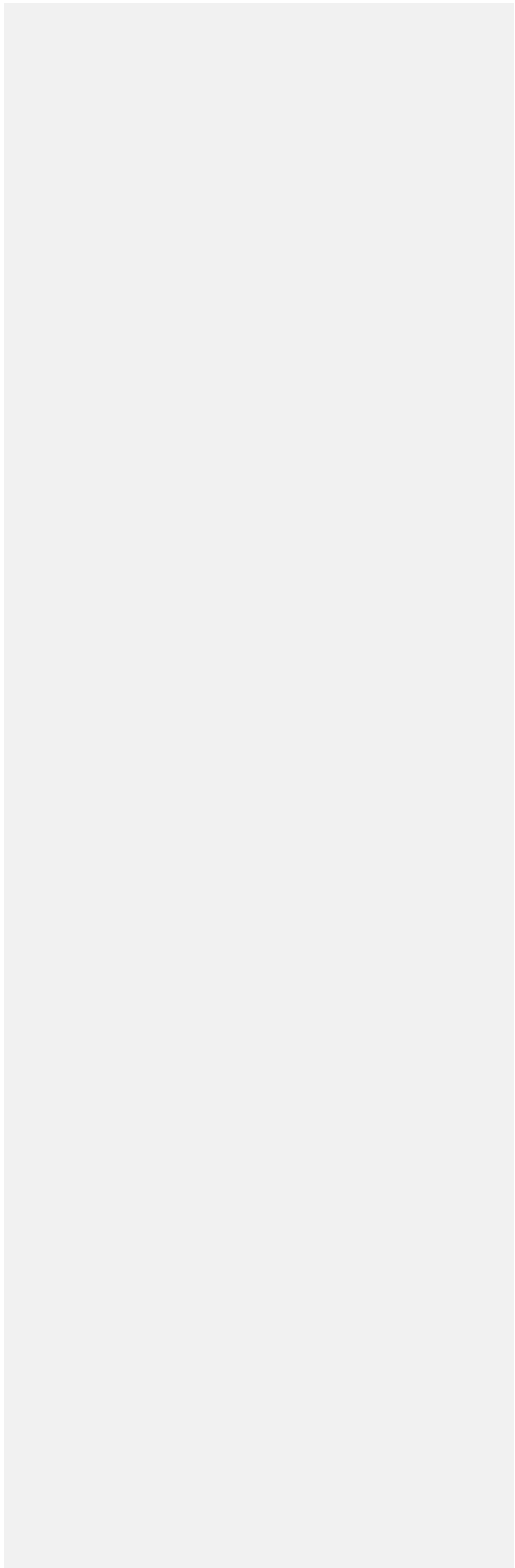
Figure 3F: Lakeland Village Neighborhoods 6 and 7



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Figure 3G: Lakeland Village Neighborhood 8



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Figure 4: Elsinore Area Plan Overlays and Policy Areas

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Figure 5: Elsinore Area Plan Meadowbrook Rural Village Overlay Highway 74 Policy Area

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Specific Plans

Specific Plans are highly customized policy or regulatory tools that provide a bridge between the General Plan and individual projects in a more area-specific manner than is possible with community-wide zoning ordinances. The specific plan is a tool that provides land use and development standards that are tailored to respond to special conditions and aspirations unique to the area being proposed for development and conservation. These tools are a means of addressing detailed concerns that conventional zoning cannot accomplish.



The authority for preparation of Specific Plans is found in the California Government Code, Sections 65450 through 65457.

Specific Plans are identified in this section as Policy Areas because detailed study and development direction is provided in each plan. Policies related to any listed specific plan can be reviewed at the Riverside County Planning Department. The four specific plans located in the Elsinore planning area are listed in Table 3, Adopted Specific Plans in the Elsinore Area Plan. Each of these specific plans is determined to be a Community Development Specific Plan.

Table 3: Adopted Specific Plans in the Elsinore Area Plan

Specific Plan	Specific Plan #
Horsethief Canyon Ranch	152
Toscana ¹	327
Renaissance Ranch	333
Colinas del Oro	364

Source: County of Riverside Planning Department.

¹ Portions of this specific plan extend into a neighboring Area Plan

Land Use

While the General Plan Land Use Element and Area Plan Land Use Map guide future development patterns in the Elsinore Area Plan, additional policy guidance is often necessary to address local land use issues that are unique to the area or that require special policies that go above and beyond those identified in the General Plan. These policies may reinforce County of Riverside regulatory provisions, preserve special lands or historic structures, require or encourage particular design features or guidelines, or restrict certain activities, among others. The intent is to enhance and/or preserve the identity, character and features of this unique area. The Local Land Use Policies section provides policies to address those land use issues relating specifically to the Elsinore area.

Local Land Use Policies

Lee Lake Community: Mixed-Use Area (MUA) Highest Density Residential (HHDR) Neighborhoods

The Lee Lake Community (see Figure 3B) is located in the Temescal Canyon, along the east side of I-15, between the freeway and Temescal Canyon Road, and south of Indian Truck Trail. It consists of two neighborhoods, the Lee Lake Neighborhood South [Neighborhood 1], and Lee Lake Neighborhood North [Neighborhood 2], which is located immediately south of Indian Wash. The Lee Lake Neighborhood North is designated as a Mixed-Use Area, with no allowance for HHDR development, and Lee Lake Neighborhood South requires 30% HHDR

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development. Although the Lee Lake Neighborhoods currently contains some light industrial development, most of the area is sparsely utilized or vacant.

Retail Commercial uses, a fire station, and parks are located nearby to the north, across I-10 via Indian Truck Trail, and Luiseno Elementary School and parks are located nearby toward the south, across I-10 via Horsethief Canyon Road. More intense light industrial development is located toward the south along Temescal Canyon Road. The Lee Lake Neighborhoods are located convenient to I-10 and Temescal Canyon Road for local and regional transportation, and near a Riverside Transit Agency bus transit line that provides convenient connections to destinations from Corona to Temecula, and to the Corona Metrolink Transit Center, which also provides the opportunity for potential links from the site or near the site to regional transit services and regional destinations.

Lee Lake Community is situated in a highly scenic setting, with spectacular views of nearby mountains to both the east and west. Lee Lake is located immediately nearby toward the east, across Temescal Canyon Road. The westerly edges of the Lee Lake Neighborhoods, located adjacent to I-15, are exposed to elevated traffic noise levels. Site designs should incorporate features to reduce freeway noise impacts, and to buffer development from nearby industrial uses.

Open space, trails, and park and recreation areas can be integrated into site development in the Lee Lake Community Neighborhoods to provide buffers and scenic recreation along the southern edges of Indian Wash, and to provide walkable destinations and internal features that promote both internal community walkability and pedestrian and bikeway access to nearby attractions off-site.

Mixed-Use (MUA) Neighborhood and Policies:

Following are descriptions of the two Mixed-Use Area neighborhood of the Lee Lake Community, and the policies that apply to each neighborhood:

The **Lee Lake South Neighborhood** [Neighborhood 1] contains about 33 gross acres (about 25 net acres) and is located between Temescal Canyon Road and I-15, immediately south of Indian Wash.

Policies:

- ELAP 7.1 Thirty percent of the Lee Lake Neighborhood shall be developed in accordance with the HHDR land use designation.
- ELAP 7.2 The portions of the Lee Lake South Neighborhood that are not developed for HHDR usage shall be primarily developed for commercial retail, office commercial, business park and light industrial, community facilities, and other uses providing opportunities for services and jobs to local residents.

The **Lee Lake North Neighborhood** [Neighborhood 2] contains about 13 gross acres (about 11 net acres), and is located adjacent to the south side of Indian Truck Trail, between the I-15 freeway and Temescal Canyon Road.

Policy:

- ELAP 7.3 The Lee Lake North Neighborhood shall contain no residential uses, but shall consist of retail and office commercial uses, to support the surrounding community with a variety of commercial services from its strategic location. Accommodations shall also be made, as appropriate, for transit,

pedestrian, and bicycle access, as appropriate, to facilitate connectivity between the neighborhood and surrounding community.

Policies Applying to both Neighborhoods of the Lee Lake Community:

- ELAP 7.4 Paseos and pedestrian and bicycle paths should be provided within the Lee Lake Community, between residential structures, community facilities, and open space areas, including between both neighborhoods and along or near both the southern edge of Indian Wash.
- ELAP 7.5 All HHDR sites should be designed to facilitate convenient pedestrian, bicycle, and other non-motorized vehicle access to the community’s schools, jobs, retail and office commercial uses, park and open space areas, trails, and other community amenities and land uses that support the community needs on a frequent and, in many cases, daily basis.
- ELAP 7.6 All new land uses, particularly residential, commercial, and public uses, including schools and parks, should be designed to provide or potentially accommodate convenient public access to alternative transportation facilities and services, including potential future transit stations, transit oasis-type shuttle systems, and/or local bus services, and local and regional trail systems.
- ELAP 7.7 All new residential and other noise-sensitive uses shall be designed to sufficiently reduce traffic noise levels from nearby roads, including I-15.
- ELAP 7.8 All new residential uses shall be designed to sufficiently reduce noise levels and other potential impacts associated with retained on-site and adjacent industrial uses.
- ELAP 7.9 Uses approved and operating under an existing valid entitlement may remain or be converted into another land use in accordance with Riverside County Ordinance No. 348 and consistent with these policies.

Mt. Palomar Nighttime Lighting

The Mount Palomar Observatory, located in San Diego County, requires darkness so that the night sky can be viewed clearly. The presence of the observatory necessitates unique nighttime lighting standards throughout the Elsinore Area Plan as shown on Figure 6, Mt. Palomar Nighttime Lighting Policy. The following policies are intended to limit light leakage and spillage that may obstruct or hinder the view. This is an excellent example of a valuable public resource that requires special treatment far beyond its immediate locale.

Policy:

- ELAP 8.1 Adhere to the lighting requirements of Riverside County for standards that are intended to limit light leakage and spillage that may interfere with the operations of the Palomar Observatory.

Circulation

The circulation system is vital to the prosperity of a community. It provides for the movement of goods and people within and outside of the community and includes motorized and non-motorized travel modes such as bicycles, trains, aircraft, automobiles and trucks. In Riverside County, the circulation system is also intended to accommodate

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a pattern of concentrated growth, providing both a regional and local linkage system between unique communities. This system is multi-modal, which means that it provides numerous alternatives to the automobile, such as transit, pedestrian systems, and bicycle facilities so that Riverside County citizens and visitors can access the region and move around within it by a number of transportation options.

As stated in the Vision and the Land Use Element, Riverside County is moving away from a growth pattern of random sprawl toward a pattern of concentrated growth and increased job creation. The intent of the new growth patterns and the new mobility systems is to accommodate the transportation demands created by future growth and to provide mobility options that help reduce the need to utilize the automobile. The circulation system is designed to fit into the fabric of the land use patterns and accommodate the open space systems.

While the following section describes the circulation system as it relates to the Elsinore Area Plan, it is important to note that the programs and policies are supplemental to, and coordinated with, the policies of the General Plan Circulation Element. In other words, the circulation system of the Elsinore Area Plan is tied to the countywide system and its long range direction. As such, successful implementation of the policies in the Elsinore Area Plan will help to create an interconnected and efficient circulation system for the entire County of Riverside.

Local Circulation Policies

Vehicular Circulation System

Environmental features both water oriented and topographic impose substantial obstacles to circulation routes; however, the Elsinore Area Plan proposes a circulation system to handle these challenges. The area is served by Railroad Canyon Road, Bundy Canyon Road, and Clinton Keith Road from the east. Temescal Canyon Road is the main arterial serving the area from the north. ~~State Route~~ Highway 74 also traverses the Area Plan in an east-west orientation.

Policies:

- ELAP 9.1 Design and develop the vehicular roadway system per Figure 7, Circulation, and in accordance with the functional classifications and standards specified in the Planned Circulation Systems section of the General Plan Circulation Element.
- ELAP 9.2 Maintain Riverside County's roadway Level of Service standards as described in the Level of Service section of the General Plan Circulation Element.

Trails System

A multi-purpose trails system is a critical part of this area plan because of the concentration of critical linkages centered here. In this sense, the trails for human use parallel the connectivity required for habitat linkages. An extensive system of proposed trails and bikeways exists within the planning area connecting the various neighborhoods with the recreational resources of the Cleveland National Forest and the regional trail system. The Elsinore Area Plan trail system is mapped in Figure 8, Trails and Bikeway System.

Policy:

ELAP 10.1 Implement the Trails and Bikeway System, Figure 8, through such means as dedication or purchase, as discussed in the Non-motorized Transportation section of the General Plan Circulation Element.



The California Scenic Highways program was established in 1963 to Preserve and protect scenic highway corridors from change which would diminish the aesthetic value of lands adjacent to highways.

Scenic Highways

Certain roadways are not only functional; they are a part of the public's ability to experience an area, especially one that offers important scenic vistas. That is the case with Interstate 15 from Corona south to the San Diego County line. It has been designated as an Eligible State Scenic Highway. [State Route/Highway 74](#) has also been designated as an Eligible State Scenic Highway. The western segment is a secondary County entrance road and will serve as a link to Orange County's system of scenic routes. The scenic highways designated within the Elsinore Area Plan are depicted on Figure 9, Scenic Highways.

Policy:

ELAP 11.1 Protect Interstate 15 and [State Route/Highway 74](#) from change that would diminish the aesthetic value of adjacent properties through adherence to the Scenic Corridors sections of the General Plan Land Use and Circulation Elements.

Community Environmental Transportation Acceptability Process (CETAP) Corridors

The population and employment of Riverside County are expected to significantly increase over the next twenty years. The CETAP was established to evaluate the need and the opportunities for the development of new or expanded transportation corridors in western Riverside County to accommodate increased growth and preserve quality of life. These transportation corridors include a range of transportation options such as highways or transit, and are developed with careful consideration for potential impacts to habitat requirements, land use plans, and public infrastructure. CETAP has identified four priority corridors for the movement of people and goods: Winchester to Temecula Corridor, East-West CETAP Corridor, Moreno Valley to San Bernardino Corridor, and Riverside County - Orange County Corridor.

The East-West CETAP Corridor may pass through the Elsinore Area Plan along [State Route/Highway 74](#), or to the north of it. This corridor could accommodate a number of transportation options, including vehicular traffic and high occupancy vehicle lanes. The Riverside County- Orange County Corridor is currently under study, but is envisioned to connect from Interstate 15 in Riverside to [State Route/Highway 241](#) in Orange County, somewhere in the range between State Route 91 and [State Route/Highway 74](#).

Elsinore Area Plan

Policies:

- ELAP 12.1 Accommodate the East-West CETAP Corridor in accordance with the CETAP section of the General Plan Circulation Element.
- ELAP 12.2 Accommodate the direction of the Riverside County-Orange County Corridor study, once it is complete.

I-15 Corridor

Interstate 15 is a major connector between the Corona/Riverside area and San Diego. This corridor could be enhanced, especially by connecting transit links, to provide a critical north-south link for transit, automobile and truck trips within and outside the County of Riverside. The capacity of this critical corridor could be expanded through such strategies as widening, high-occupancy vehicle lanes, dedicated truck lanes, and transit improvements, such as exclusive express buses. Infrastructure put in place along with development in this area plan should support all modes of transit along this corridor.

Policies:

- ELAP 13.1 Require projects to be reviewed for the provision of transit support facilities (including bus turnouts, signage, benches, shelters, etc.) along arterial streets and local transit service routes.
- ELAP 13.2 Consider the following regional and community wide transportation options when developing transportation improvements in the Elsinore Area Plan:
- a. Construct a new interchange on Interstate 15 at Horsethief Canyon Road.
 - b. Develop regional transportation facilities and services (such as high-occupancy vehicle lanes and express bus service), which will encourage the use of public transportation and ridesharing for longer-distance trips.
- ELAP 13.3 Require each proposed Specific Plan, and major commercial and industrial projects consisting of 20 acres or larger, to be evaluated for the provision of a park-and-ride facility.

Figure 5: Elsinore Area Plan Mt. Palomar Nighttime Lighting Policy Area

Elsinore Area Plan

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Figure 6: Elsinore Area Plan Circulation

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Figure 7: Elsinore Area Plan Trails and Bikeway System

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Figure 8: Elsinore Area Plan Scenic Highway

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Multipurpose Open Space

The Elsinore area contains an unusually rich concentration of open space resources, for habitat, recreation and scenic purposes, hence the label of multipurpose. The point is that open space is really a part of the public infrastructure and should have the capability of serving a variety of needs and diversity of users. The importance of the resources here means that they require thoughtful preservation and, in some cases, restoration. In many cases, the focus here must be on establishing and maintaining vital linkages, without which the vital habitat and recreational potential of this area would be severely compromised. This Multipurpose Open Space section is a critical component of the character of the County of Riverside and of the Elsinore Area Plan. Preserving the scenic background and natural resources here gives meaning to the remarkable environmental setting portion of the overall Riverside County Vision. Not only that: these open spaces also help define the edges of and separation between communities, which is another important aspect of the Vision.

In this area plan, the natural characteristics are quite dominant. In addition to their extensive basic supply value, they offer design opportunities for quality development. Achieving a desirable end state of valued local open space to benefit residents and visitors will require sensitive design attention in laying out development proposals and linkages to make the open space system work to its optimum.

Local Open Space Policies

Watersheds, Floodplains, and Watercourse Policies

The Elsinore Area Plan contains a major portion of the Santa Margarita River watershed, which includes Murrieta Creek. This watershed, and its included watercourses, provide a truly unique habitat for flora and fauna of statewide significance. The watercourses provide corridors through developed land as well as linking open spaces outside of development areas. This allows wildlife the ability to move from one locale to another without crossing developed land. The following policies preserve and protect these important watershed functions.

Policy:

ELAP 14.1 Protect the Santa Margarita watershed and habitat, and provide recreational opportunities and flood protection through adherence to the policies found in the Open Space, Habitat, and Natural Resource Preservation section of the General Plan Land Use Element and the Environmentally Sensitive Lands,

“

The open space system and the methods for its acquisition, maintenance, and operation are calibrated to its many functions: visual relief, natural resources protection, habitat preservation, passive and active recreation, protection from natural hazards, and various combinations of these purposes. This is what is meant by a multipurpose open space system.

”

- RCIP Vision



A watershed is the entire region drained by a waterway that flows into a lake or reservoir or the ocean. It is the total area above a given point on a stream that contributes water to the flow at that point, and the topographic dividing line from which surface streams flow in two different directions. Clearly, watersheds are not just water. A single watershed may include a wide variety of resources and environments.

Elsinore Area Plan

Floodplain and Riparian Area Management, Wetlands, and Open Space, Parks and Recreation sections of the Multipurpose Open Space Element.

Mineral Extraction

There are significant areas of mineral resource extraction within the Elsinore Area Plan. The area contains regionally important aggregate and clay resources, as well as non-regionally important mineral resources. Most of these resources are currently being extracted or are being held in reserve for future extraction. Compatibility with surrounding land uses, potential noxious impacts, surface runoff management, and the future reclamation of the sites must be considered for all existing and proposed mineral extraction areas.

Policies:

- ELAP 15.1 Protect the economic viability of mineral resources as well as the life and property of Elsinore Area Plan residents through adherence to the Mineral Resources section of the General Plan Multipurpose Open Space Element.
- ELAP 15.2 Avoid mineral resource extraction within the Temescal Wash Policy Area, which contains viable riparian habitat, in favor of areas containing very sparse or non-existent riparian habitat.
- ELAP 15.3 Require a biologically designed and professionally implemented revegetation program as part of reclamation plans, where avoidance is not feasible.
- ELAP 15.4 Require hydrologic studies by a qualified consultant as part of the environmental review process for all proposed surface mining permits within or adjacent to the Temescal Wash Policy Area. This shall include proper management of surface run-off.

Oak Tree Preservation

The Elsinore Area Plan contains significant oak woodland areas. Oak woodlands should be protected to preserve habitat and the character of the area.

Policy:

- ELAP 16.1 Protect viable oak woodlands through adherence to the Oak Tree Management Guidelines adopted by Riverside County and the Vegetation section of the Multipurpose Open Space Element of the General Plan.



For further information on the MSHCP please see the Multipurpose Open Space Element of the General Plan.

Multiple Species Habitat Conservation Plan

Regional resource planning to protect individual species such as the Stephens Kangaroo Rat has occurred in Riverside County for many years. Privately owned reserves and publicly owned land have served as habitat for many different species. This method of land and wildlife preservation proved to be piecemeal and disjointed, resulting in islands of reserve land without corridors for species migration and access. To address these issues of wildlife health and habitat sustainability, the Western Riverside County Multiple Species Habitat Conservation Plan (WRC MSHCP) was developed by the County of Riverside

and adopted by the County of Riverside and other plan participants in 2003. Permits were issued by the Wildlife Agencies in 2004. The WRC MSHCP comprises a reserve system that encompasses core habitats, habitat linkages, and wildlife corridors outside of existing reserve areas and existing private and public reserve lands into a single comprehensive plan that can accommodate the needs of species and habitat in the present and future.

WRC MSHCP Program Description

The Endangered Species Act prohibits the “taking” of endangered species. Taking is defined as “to harass, harm, pursue, hunt, shoot, wound, kill, trap, capture, or collect” listed species. The Wildlife Agencies have authority to regulate this take of threatened and endangered species. The intent of the WRC MSHCP is for the Wildlife Agencies to grant a take authorization for otherwise lawful actions that may incidentally take or harm species outside of reserve areas, in exchange for supporting assembly of a coordinated reserve system. Therefore, the WRC MSHCP allows the County of Riverside to take plant and animal species within identified areas through the local land use planning process. In addition to the conservation and management duties assigned to the County of Riverside, a property owner-initiated habitat evaluation and acquisition negotiation process has also been developed. This process is intended to apply to property that may be needed for inclusion in the WRC MSHCP Reserve or subjected to other WRC MSHCP criteria.

Key Biological Issues

The habitat requirements of the sensitive and listed species, combined with sound habitat management practices, have shaped the following policies. These policies provide general conservation direction.

Policies:

- ELAP 17.1 Protect sensitive biological resources in the Elsinore Area Plan through adherence to policies found in the Multiple Species Habitat Conservation Plans, Environmentally Sensitive Lands, Wetlands, and Floodplain and Riparian Area Management sections of the General Plan Multipurpose Open Space Element.
- ELAP 17.2 Provide for connection between Santa Ana Mountains, Temescal Wash and foothills north of Lake Elsinore; existing connections are at Indian Truck Trail (buffer along Canyon Creek), Horsethief Canyon, and open upland areas southwest of Alberhill.
- ELAP 17.3 Provide northwest-southeast connection along hills between Estelle Mountain and Sedco Hills, primarily for California gnatcatchers, but also other sage scrub species.



The Wildlife Agencies include The United States Fish and Wildlife Service (USFWS) and the California Department of Fish and Wildlife (CDFW)



The following sensitive, threatened and endangered species, covered under the MSHCP, may be found within this area plan:

- Bell’s sage sparrow
- California gnatcatcher
- Orange-throated whiptail
- Loggerhead shrike
- San Diego ambrosia
- Bobcat
- Quino checkerspot butterfly
- Munz’s onion
- Many-stemmed dudleya
- Southwestern willow flycatcher
- Least Bell’s vireo
- Slender-horned spineflower

Elsinore Area Plan

- ELAP 17.4 Conserve clay soils supporting sensitive plants such as Munz's onion, many-stemmed dudleya, small-flowered morning glory and Palmer's grapplinghook. (There is a Munz's onion population of approximately 7,500 heads in Alberhill.)
- ELAP 17.5 Conserve wetlands including Temescal Wash, Collier Marsh, Alberhill Creek, Wasson Creek, and the lower San Jacinto River, (including marsh habitats and maintaining water quality).
- ELAP 17.6 Maintain upland habitat connection between North Peak Conservation Bank, Steele Peak, and Bureau of Land Management (BLM) lands.
- ELAP 17.7 Conserve Engelmann Oak Woodlands.
- ELAP 17.8 Conserve sensitive plants, including Parry's spineflower, prostrate spineflower, Payson's jewelflower, smooth tarplant, slender-horned spineflower, Couldte's matijila poppy, Palomar monkeyflower, little mousetail, vernal barley, San Jacinto Valley crownscale, Coulter's goldfields, heart-leaved pitcher sage, and the Quino checkerspot butterfly.
- ELAP 17.9 Conserve Travers-Willow-Domino soil series.
- ELAP 17.10 Conserve foraging habitat adjacency for raptors, sage scrubbed-grassland ecotone.
- ELAP 17.11 Conserve habitat in Sedco Hills to maintain connection between Granite Hills and Bundy Canyon Road.
- ELAP 17.12 Provide for connection across [State Route Highway 74](#) for birds and land species.
- ELAP 17.13 For Wasson Creek, maintain north-south linkage at least 750 feet wide from Wasson Creek to North Peak.
- ELAP 17.14 South of Wasson Creek, development should be limited to western and eastern slopes.

Hazards

Hazards are natural and manmade conditions that must be respected if life and property are to be protected as growth and development occur. As the ravages of wildland fires, floods, dam failures, earthquakes and other disasters become clearer through the news, public awareness and sound public policy combine to require serious attention to these conditions. Portions of the Elsinore Area Plan may be subject to hazards such as flooding, dam inundation, seismic occurrences, and wildland fire. These hazards are depicted on the hazards maps, Figure 10 to Figure 14. These hazards are located throughout the Elsinore area and produce varying degrees of risk and danger. Some hazards must be avoided entirely while the potential impacts of others can be mitigated by special building techniques. The following policies provide additional direction for relevant issues specific to the Elsinore Area Plan.

Local Hazard Policies

Flooding and Dam Inundation

Temescal Wash, Murrieta Creek, and the San Jacinto River, as well as Lake Elsinore, pose significant flood hazards within the Elsinore Area Plan. Dam failure of the Railroad Canyon Dam at Canyon Lake would cause flooding in the plan area. Refer to Figure 10, Flood Hazard Zone for a depiction of flood hazards in the Elsinore area.

Policies:

- ELAP 18.1 Adhere to the flood proofing and flood protection requirements of the Riverside County Flood Control and Water Conservation District.
- ELAP 18.2 Protect proposed development projects that are subject to flood hazards, surface ponding, high erosion potential or sheet flow by requiring submittal to the Riverside County Flood Control and Water Conservation District for review.
- ELAP 18.3 When possible, create flood control projects that maximize multi-recreational use and water recharge.
- ELAP 18.4 Protect life and property from the hazards of flood events through adherence to the policies identified in the Flood and Inundation Hazards Abatement section of the General Plan Safety Element.

Wildland Fire Hazard

The plan area contains a number of unique features and communities that are subjected to a high risk of fire hazards, including the Cleveland National Forest, Cleveland Ridge, Warm Springs and Meadowbrook. Methods to address this hazard include techniques such as avoidance of building in high-risk areas, creating setbacks that buffer development from hazard areas, maintaining brush clearance to reduce potential fuel, establishing low fuel landscaping, and utilizing fire-resistant building techniques. In still other cases, safety oriented organizations such as the Fire Safe Council can provide assistance in educating the public and promoting practices that contribute to improved public safety. Refer to Figure 11, Fire Hazard Severity Zone.

Policy:

- ELAP 19.1 All proposed development located within High or Very High Fire Hazard Severity Zones shall protect life and property from wildfire hazards through adherence to policies identified in the Fire Hazards (Building Code and Performance Standards), Wind-Related Hazards and General and Long-Range Fire Safety Planning sections of the General Plan Safety Element.



Fire Fact:
Santa Ana winds create a special hazard. Named by the early settlers at Santa Ana, these hot, dry winds heighten the fire danger throughout Southern California.



Liquefaction occurs primarily in saturated, loose, fine to medium-grained soils in areas where the groundwater table is within about 50 feet of the surface. Shaking causes the soils to lose strength and behave as liquid. Excess water pressure is vented upward through fissures and soil cracks and a water-soil slurry bubbles onto the ground surface. The resulting features are known as "sand boils, sand blows" or "sand volcanoes."

Liquefaction-related effects include loss of bearing strength, ground oscillations, lateral spreading, and flow failures or slumping.

Seismic

The Elsinore fault runs north-south through the middle of the plan area. Threats from seismic events include ground shaking, fault rupture, liquefaction, and landslides. The use of specialized building techniques, the enforcement of setbacks from faults, and practical avoidance measures will help to mitigate the potentially dangerous circumstances. Refer to Figure 13, Seismic Hazards, for the location of faults within the Elsinore Area.

Policy:

ELAP 20.1 Protect life and property from seismic-related incidents through adherence to the policies in the Seismic Hazards and Geologic Hazards section of the General Plan Safety Element.

Slope

Many areas within the Elsinore Area Plan, depicted on Figure 14, Steep Slope, contain steep slopes that require special development standards and care to prevent erosion and landslides, preserve significant views and minimize grading and scarring. Additionally, the ridgelines of the Santa Ana Mountains and Gavilan and Sedco Hills provide a significant visual resource for users of the Interstate 15 corridor and occupants of the valley floor.

Policies:

ELAP 21.1 Identify and preserve the ridgelines that provide a significant visual resource for Elsinore through adherence to the Hillside Development and Slope section of the General Plan Land Use Element and the Scenic Resources section of the Multipurpose Open Space Element.

ELAP 21.2 Prohibit building sites on the Gavilan Hills Ridgeline. Projects proposed within this area shall be evaluated on a case by case basis to ensure that building pad sites are located so that buildings and roof tops do not project above the ridgeline as viewed from Interstate 15.

ELAP 21.3 Protect life and property and maintain the character of the Elsinore area through adherence to the Slope and Soil Instability Hazards section of the General Plan Safety Element, the Hillside Development and Slope section of the General Plan Land Use Element, and the Rural Mountainous land use designation.

Figure 9: Elsinore Area Plan Flood Hazard Zone

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Figure 10: Elsinore Area Plan Fire Hazard Severity Zone

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Figure 11: Elsinore Area Plan Historic Wildfire Areas

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Figure 12: Elsinore Area Plan Seismic Hazards

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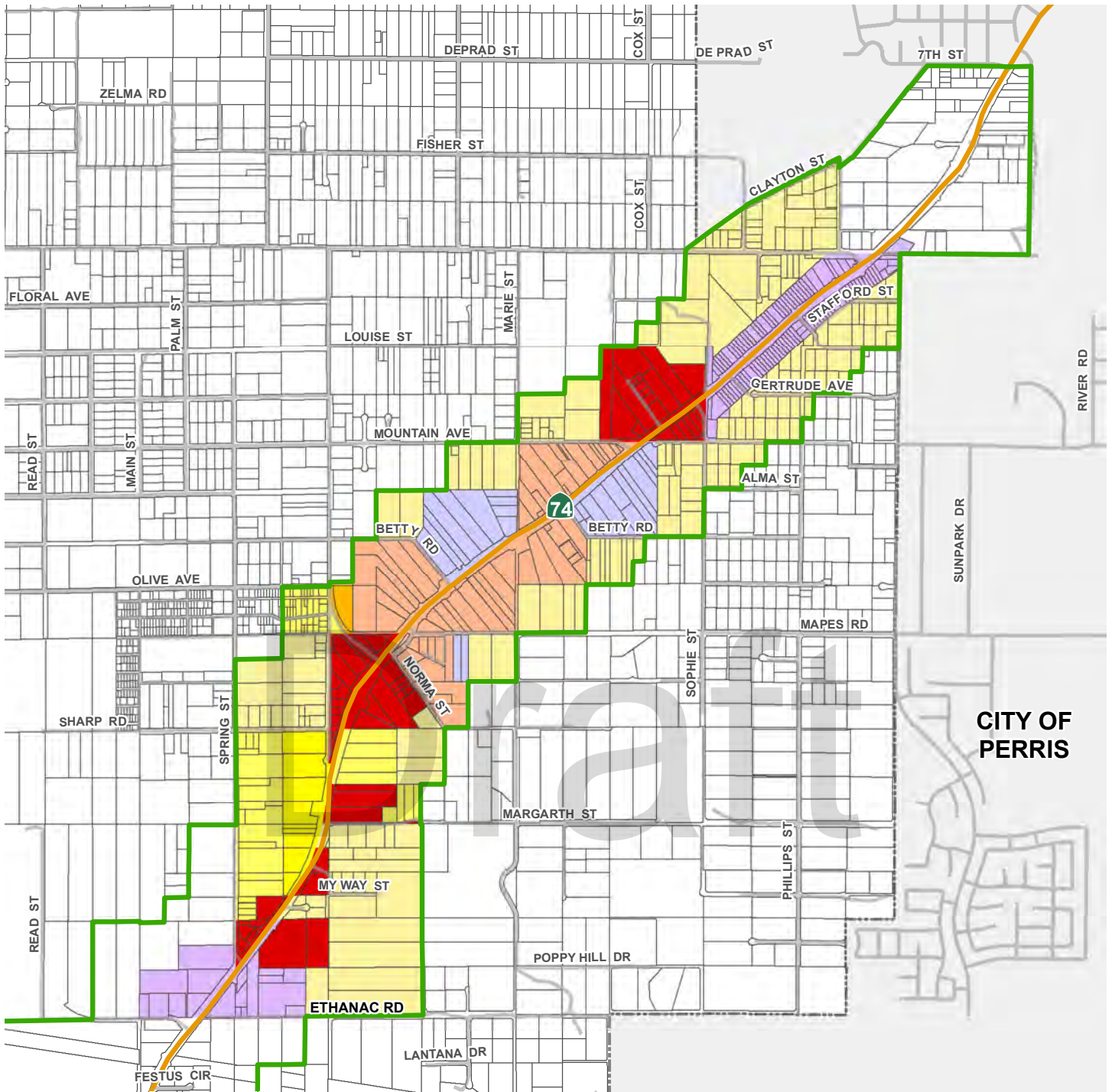
Figure 13: Elsinore Area Plan Steep Slope

Elsinore Area Plan

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Figure 14: Elsinore Area Plan Slope Instability

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**CITY OF
PERRIS**

Prepared By: Riverside County Planning Department
Source: Riverside County

HIGHWAY 74 POLICY AREA Proposed General Plan Land Use

DRAFT EIR MAP 1A

- | | |
|------------------------------|----------------------------|
| Highway 74 | Medium Density Residential |
| Highway 74 Policy Area | High Density Residential |
| City Boundary | Commercial Retail |
| COMMUNITY DEVELOPMENT | Light Industrial |
| Very Low Density Residential | Business Park |
| Low Density Residential | Mixed-Use Area |

12/21/2021

Path: C:\Users\rvlajp\Desktop\HWY 79 POLICY AREA LAND USE MVAP.mxd



0 0.25 0.5 Miles

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



April 25, 2023

Evan Langan, Project Planner
County of Riverside Planning Department
4080 Lemon Street, 12th Floor
Riverside CA 92501

CHAIR
Steve Manos
Lake Elsinore

VICE CHAIR
Russell Betts
Desert Hot Springs

COMMISSIONERS

John Lyon
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Moreno Valley

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Director
Paul Rull

Simon Housman
Jackie Vega
Barbara Santos

County Administrative Center
4080 Lemon St., 14th Floor.
Riverside, CA 92501
(951) 955-5132

www.rcaluc.org

RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW – DIRECTOR’S DETERMINATION

File No.: ZAP1551MA22
Related File No.: SP00260A4 (Specific Plan Amendment), GPA210116 (General Plan Amendment), CZ210130 (Change of Zone), TTM38131 (Tentative Tract Map)
APN: 457-120-065, 457-130-015, 457-130-016, 457-130-017, 457-130-018
Airport Zone: Zone E

Dear Mr. Langan:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Resolution No. 2015-01 (as extended by Resolution No. 2020-01) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed County of Riverside Case Nos. SP00260A4 (Specific Plan Amendment), GPA210116 (General Plan Amendment), CZ210130 (Change of Zone), TTM38131 (Tentative Tract Map), a proposal to divide 52.4 acres into 201 single family residential lots located northerly of Highway 74, southerly of Alicante Drive, easterly Charina Lane, and westerly of Juniper Flats Road. The applicant also proposes to amend 5.4 acres of the site’s General Plan land use designation from Medium Density Residential to Commercial Retail, and amend 17.5 acres of the site from Medium Density Residential to Medium High Density Residential. The applicant also proposes to change the zoning of 17.5 acres (new PA48) from Rural Residential to Specific Plan No. 260, and to adopt amended Zoning Ordinance standards for existing PA 45 and starts for new PA’s 47, 48, and 49, and define new and/or modified boundaries for PA’s 45, 47, 48 and 49. The applicant also proposes to amend Specific Plan 260 1) modify the boundaries of PA 45 by removing a portion located east of Leon Road and west of Juniper Flats Road totaling 34.69 acres to create new PA 47 and PA 49. The amended PA 45 will be bordered by Leon Road to the east, Watson Road to the south and Alicante Drive to the north, will continue to have a Planning Area Land Use of Medium Density Residential (MDR), a new area of 27.3 acres, and allowable density from 3.6 to 4.2 dwelling units/acre. New PA 47 will be bound by Leon Road to the west and Juniper Flats Road to the east, will continue to have a Planning Area Land Use of Medium Density Residential (MDR), have an area of 29.5 acres, and an allowable density from 3.6 to 3.8 dwelling units/ acre. New PA 49 is located at the northwest corner of Alicante Drive and Juniper Flats Road, will have a new Planning Area Land Use designation of Commercial Retail (CR) and an area of 5.4 acres. 2) Expand the boundaries of Specific Plan No. 260 with the incorporation of 17.5 acres located south of Watson Road, east of Leon Road, and west of Juniper Flats Road for the establishment of new PA 48 with a Planning Area Land Use of Medium High Density Residential (MHDR) and allowed density of 5.1 dwelling units per acre.

The project is located within Compatibility Zone E of March Air Reserve Base/Inland Port Airport Influence Area, where Zone E does not restrict residential density.

Although the project is located within the March Air Reserve Base/Inland Port Airport Influence Area, the nearest runway is actually Runway 5-23 at Hemet-Ryan Airport. The elevation of Runway 5-23 at Hemet-Ryan Airport is approximately 1,499 feet above mean sea level (AMSL) at its westerly terminus. At a distance of 27,048 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures taller than 200 feet in height. The project proposes a maximum structure height of 50 feet. At a distance of 27,048 feet from the runway, FAA OES review for height/elevation was not required.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT**, with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, provided that the County of Riverside applies the following recommended conditions:

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, outdoor production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Highly noise-sensitive outdoor nonresidential uses.
 - (f) Any use which results in a hazard to flight, including physical (e.g., tall objects), visual, and electronic forms of interference with the safety of aircraft operations.
3. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property.
4. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the stormwater basins that would provide food or cover for birds would be incompatible with airport operations and shall

not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the stormwater basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the stormwater basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin

If you have any questions, please contact me at (951) 955-6893.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Paul Rull, ALUC Director

Attachments: Notice of Airport in Vicinity

cc: Richland Communities (applicant)
T&B Planning (representative)
JHB Colony Investments, LLC (property owner)
Gary Gosliga, March Inland Port Airport Authority
Major. David Shaw, Base Civil Engineer, March Air Reserve Base
ALUC Case File

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NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS

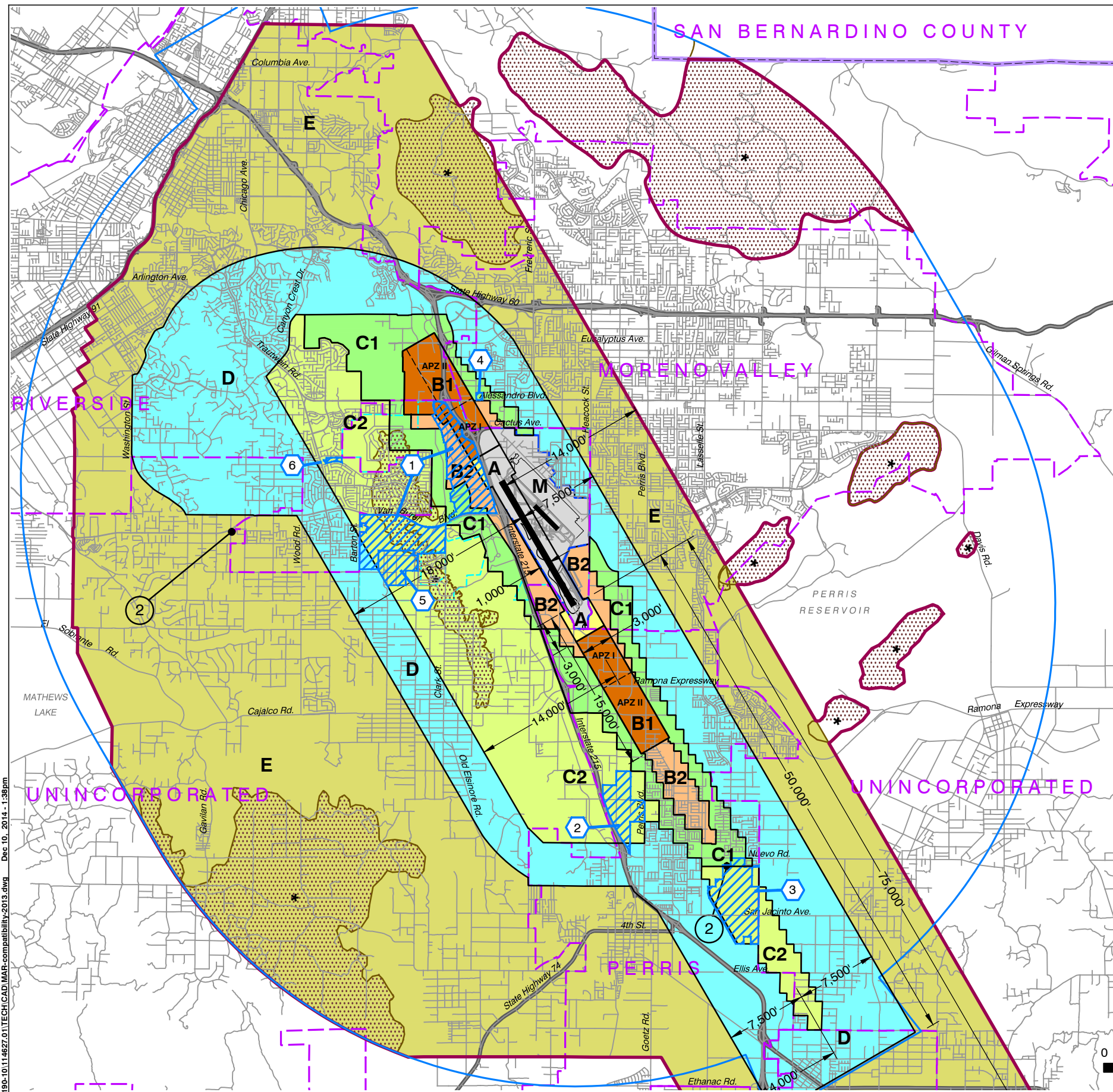
PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



LEGEND

Compatibility Zones

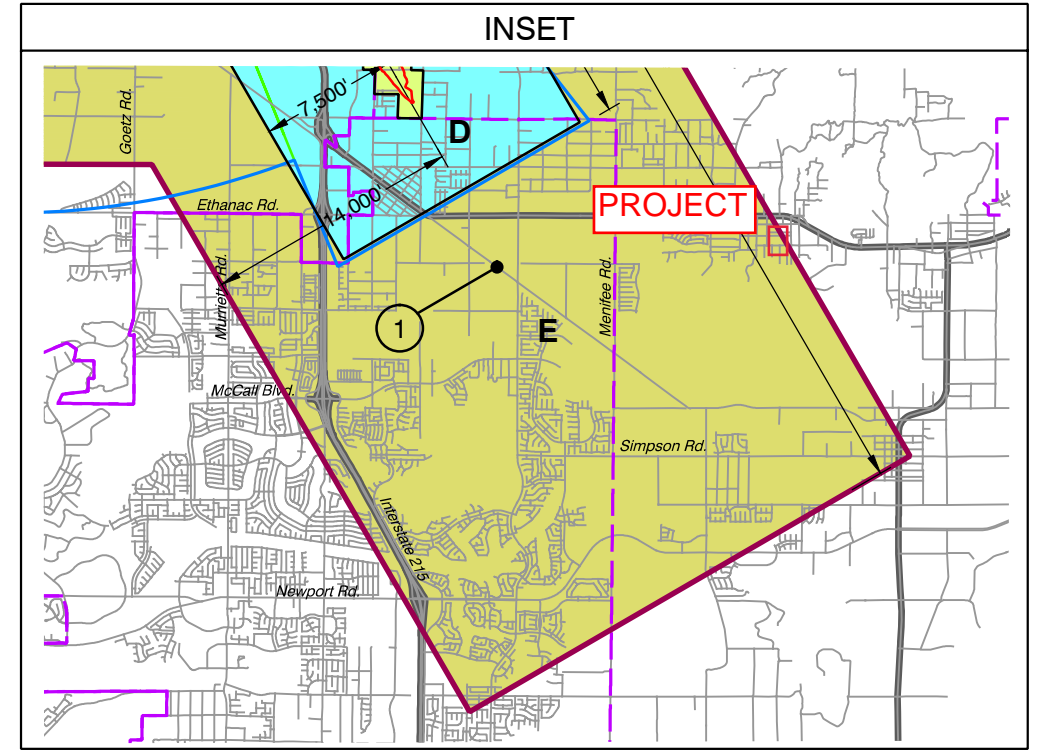
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

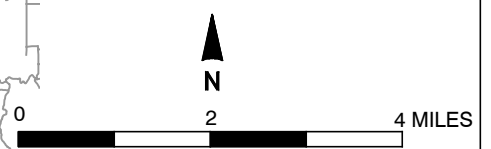
- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



**Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)**

Note:
All dimensions are measured from runway ends and centerlines.



Base map source: County of Riverside 2013

SEE INSET AT RIGHT

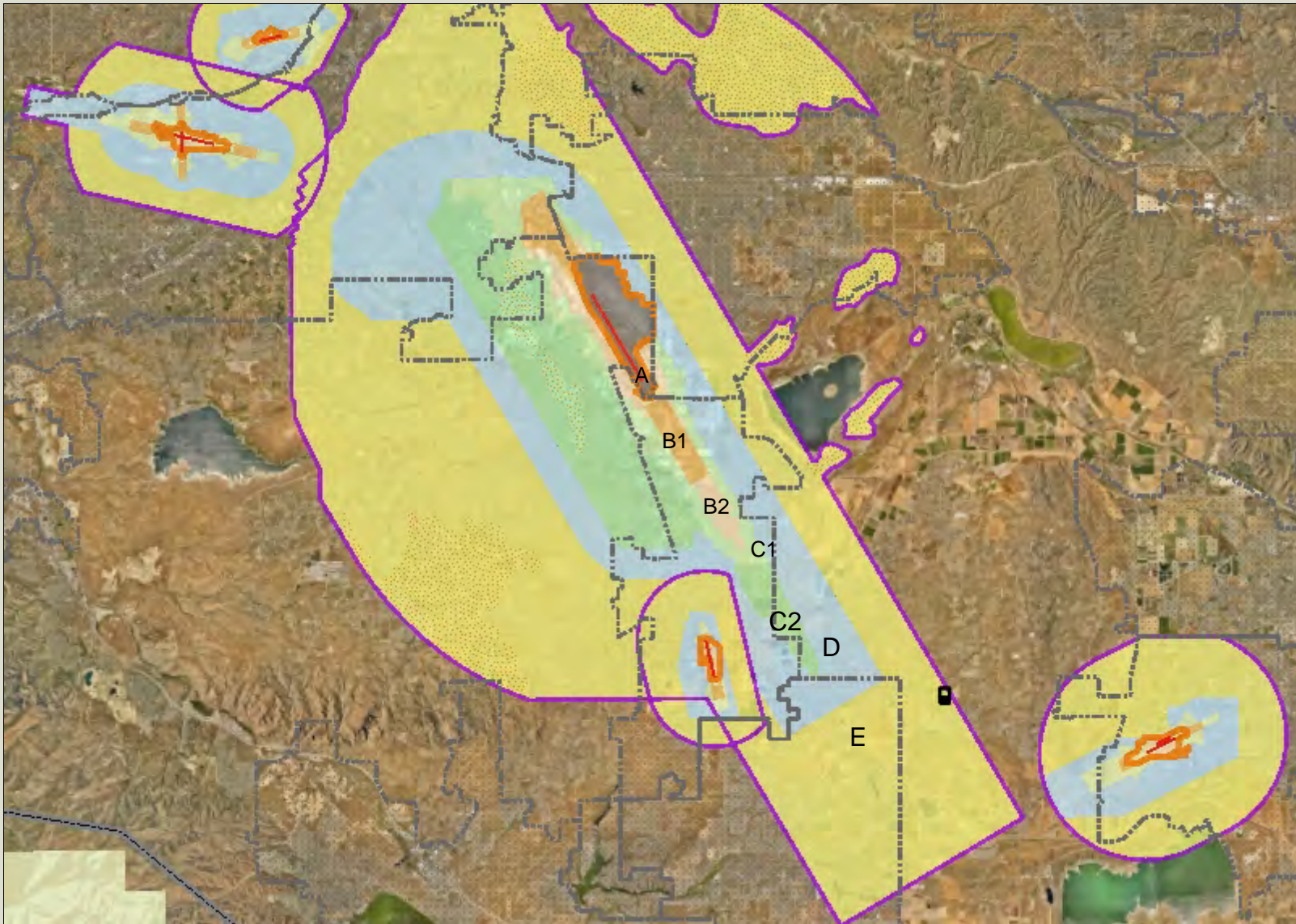
X:\18190-10\114627\01\TECH\CAD\MAR-compatibility\2013.dwg Dec 10, 2014 - 1:38pm

Prepared by Mead & Hunt, Inc. (June 2013)

Map MA-1

**Compatibility Map
March Air Reserve Base / Inland Port Airport**

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

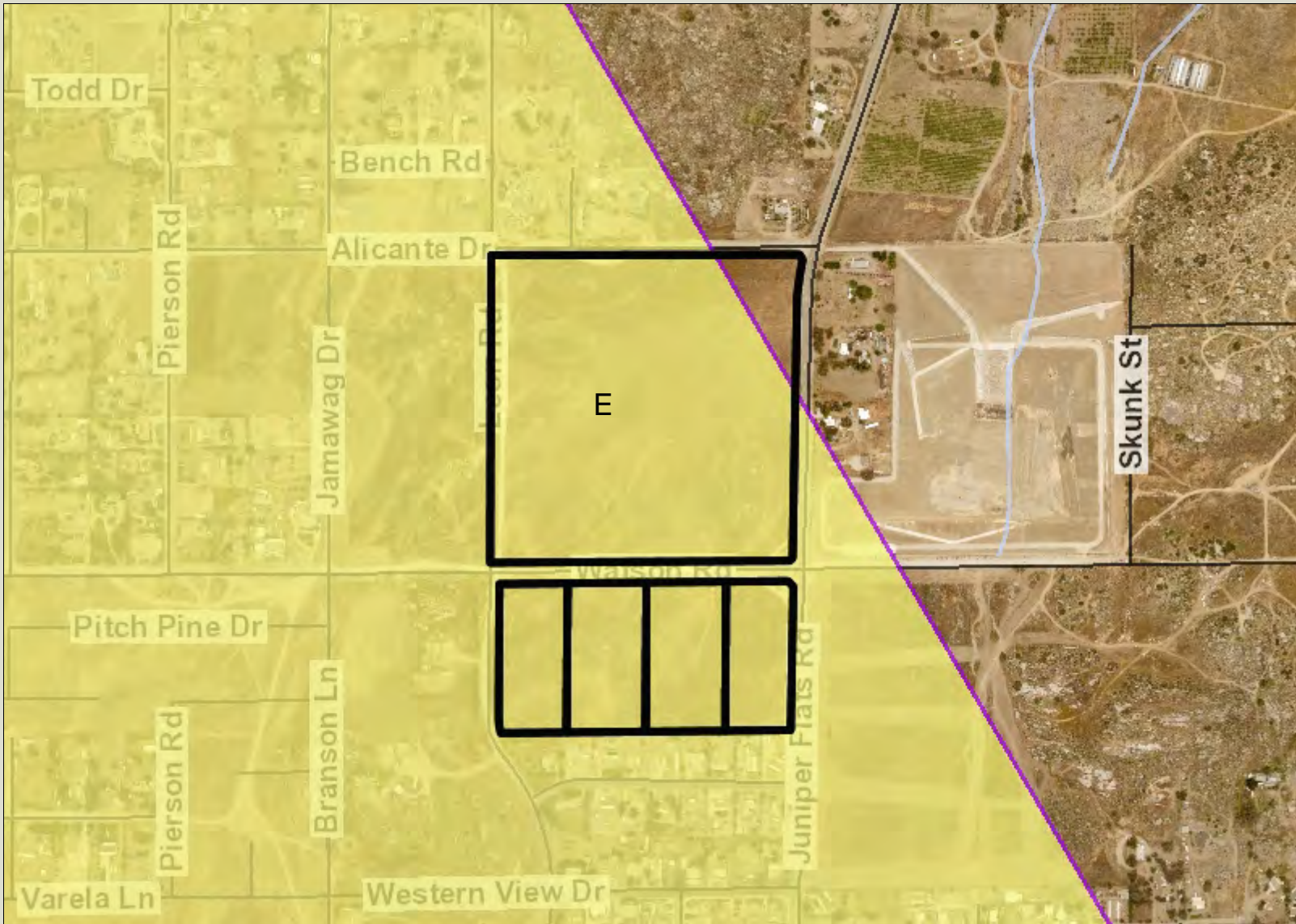


REPORT PRINTED ON... 12/21/2022 7:56:03 AM

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Notes

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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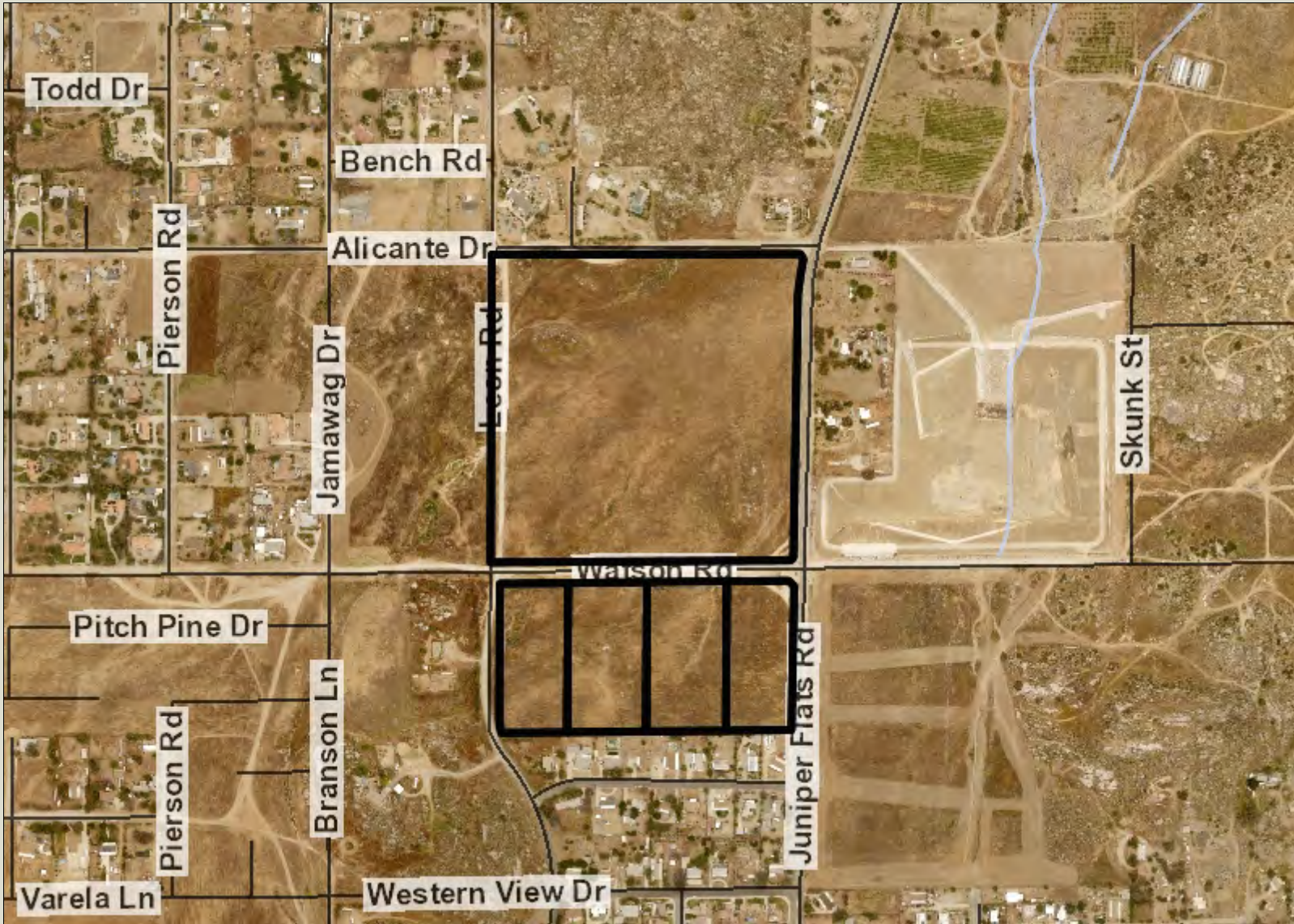


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Notes

Map My County Map



Legend

- County Centerline Names
- County Centerlines
- Blueline Streams
- ⋮ City Areas
- ⋮ World Street Map



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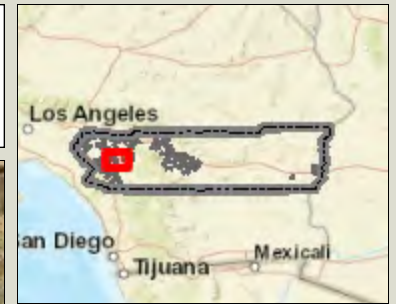
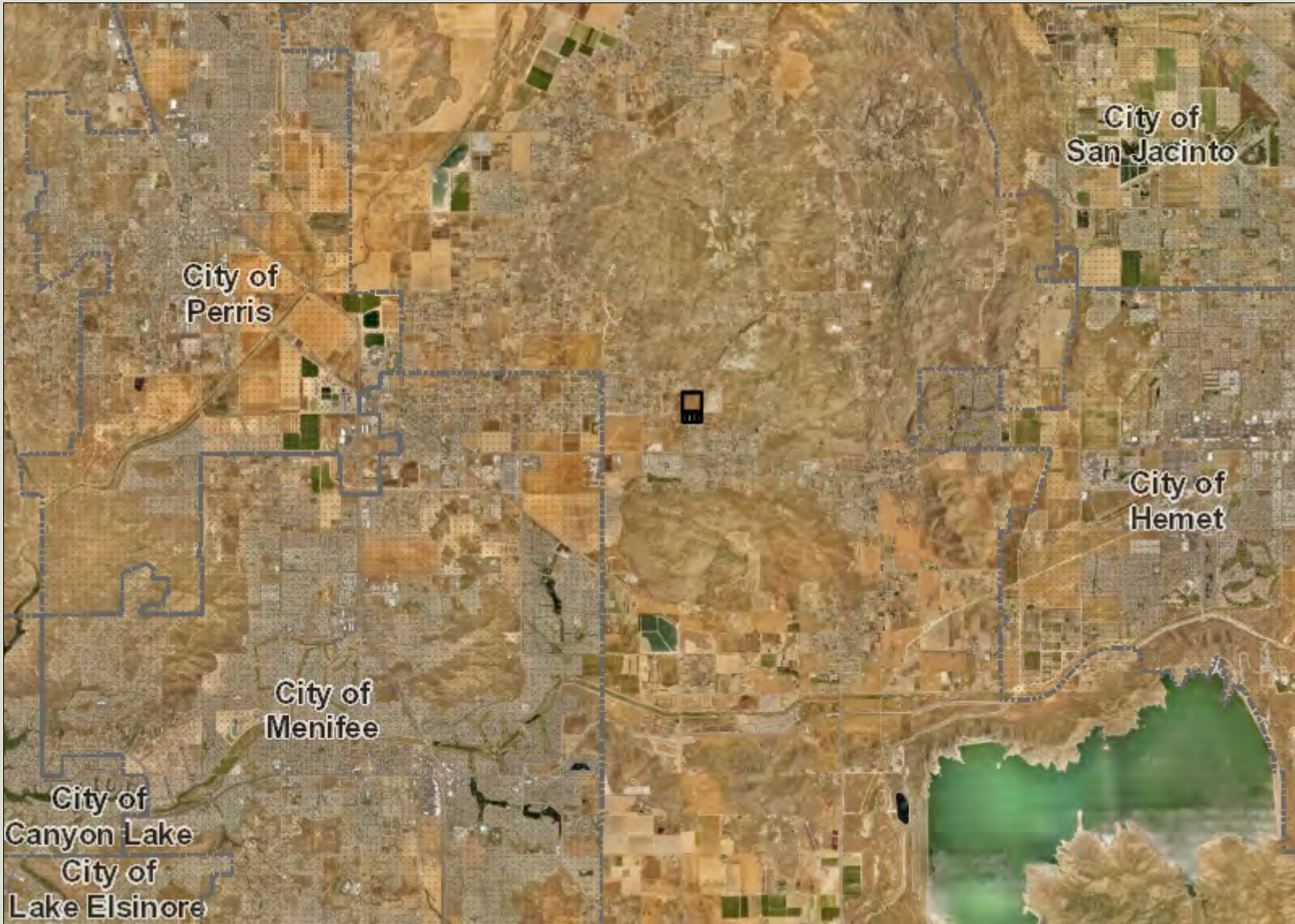
0 770 1,539 Feet

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
© Riverside County GIS

Notes

Map My County Map



Legend

-  City Areas
-  World Street Map



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

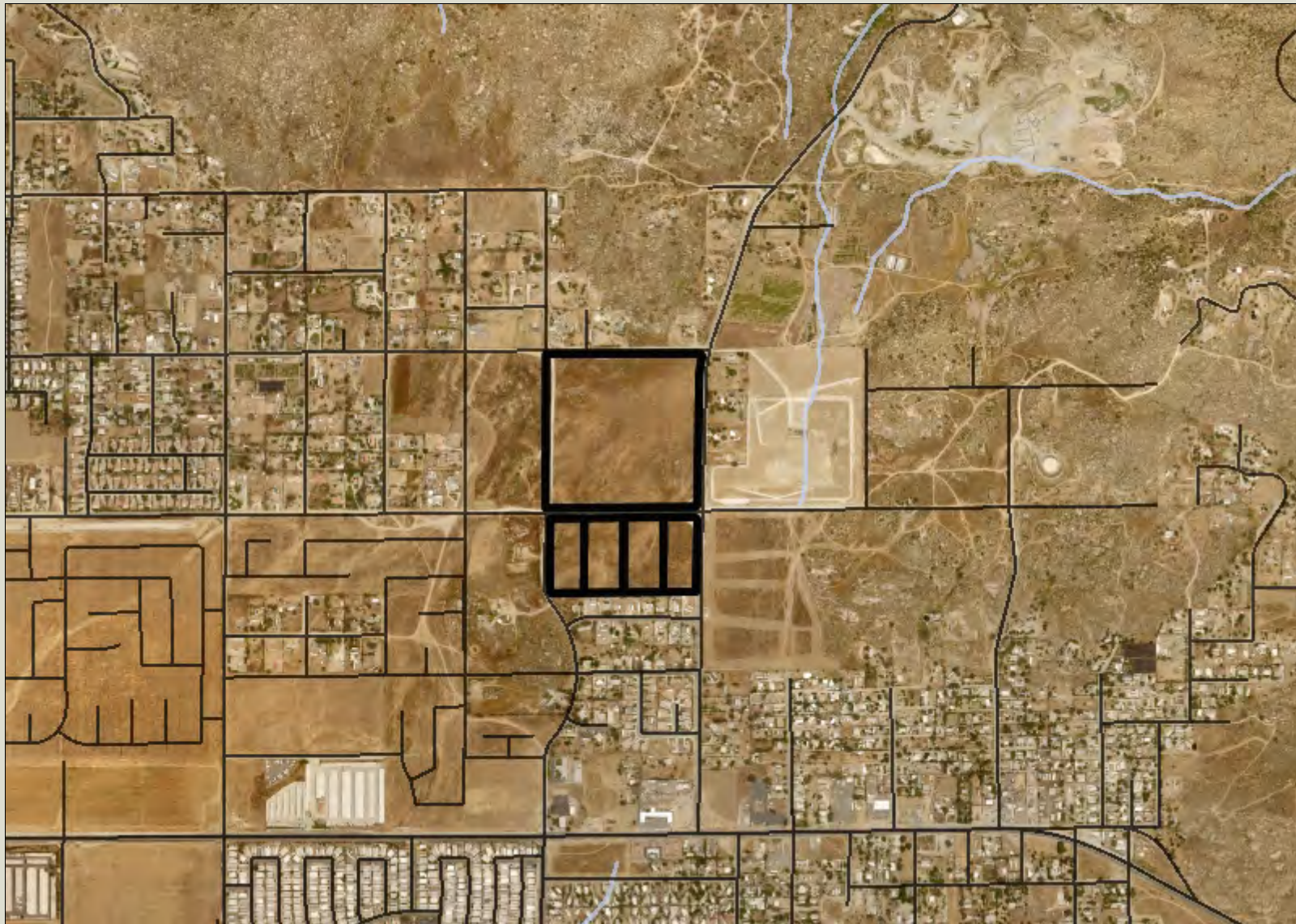
Notes



REPORT PRINTED ON... 12/21/2022 7:58:12 AM

© Riverside County GIS

Map My County Map



Legend

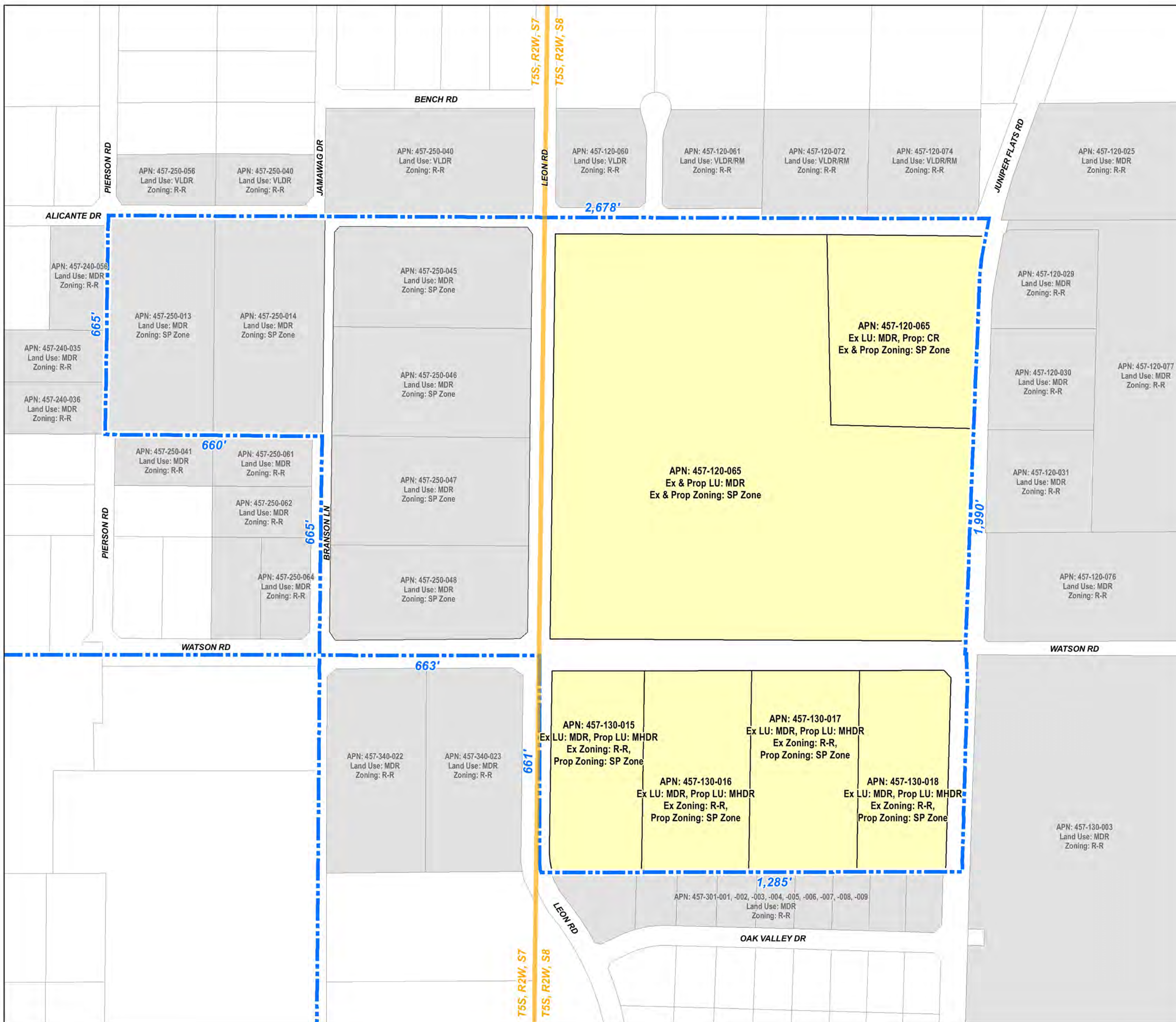
- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



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Notes





APPLICANT:
 Richland Communities
 Attn: Brian Hardy
 3161 Michelson Drive, Suite 425
 Irvine, CA 92612

LAND OWNER:
 Richland Communities
 Attn: Brian Hardy
 3161 Michelson Drive, Suite 425
 Irvine, CA 92612

LEGAL DESCRIPTION:
 See Attached.

EXISTING LAND USE DESIGNATION(S):
 Medium Density Residential (MDR)

PROPOSED LAND USE DESIGNATION(S):
 Medium Density Residential (MDR), Medium High Density Residential (MHDR), Commercial Retail (CR)

EXISTING ZONING:
 Rural Residential (R-R), SP Zone

PROPOSED ZONING:
 SP Zone

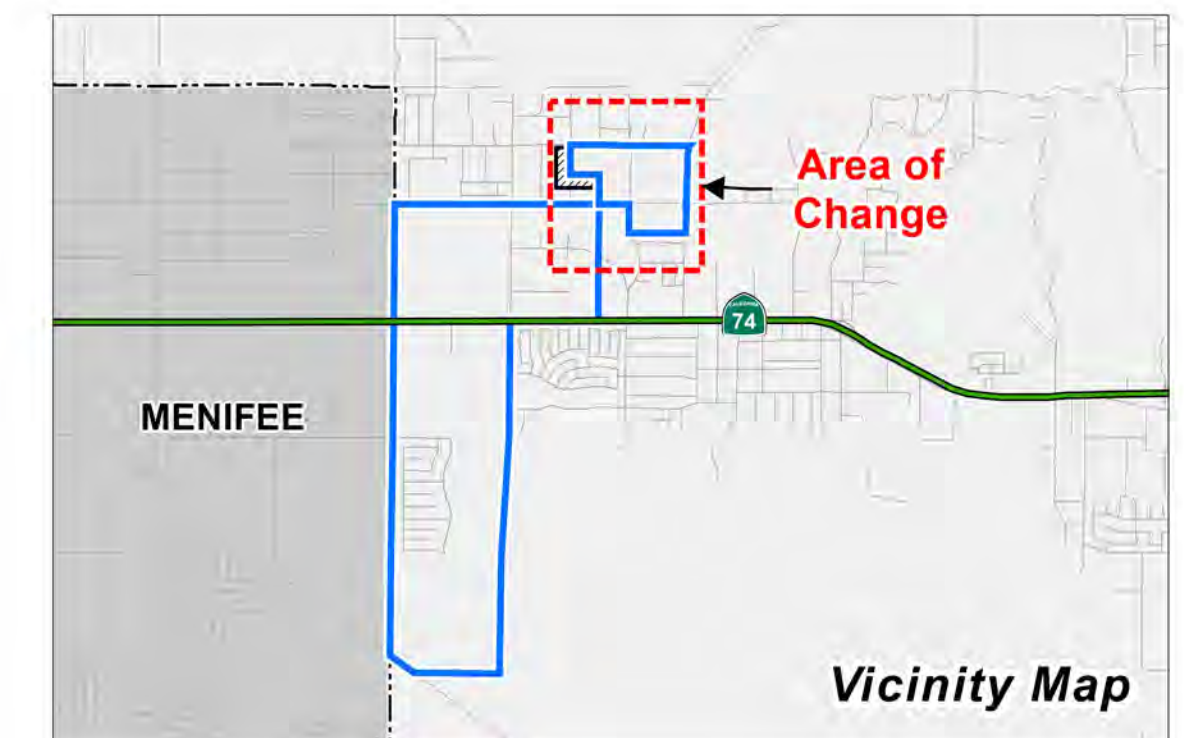
AMENDMENT DESCRIPTION:
 Amend the Harvest Valley/Winchester Area Plan from Medium Density Residential to Medium Density Residential, Medium High Density Residential, Commercial Retail

ASSESSOR'S PARCEL NUMBER(S):
 457-120-065, 457-130-015, 457-130-016, 457-130-017, 457-130-018)

UTILITIES:
 Water: EMWD
 Sewer: EMWD
 Gas: So Cal Gas
 Electric: So Cal Edison
 Telephone: Frontier Communications

ACRES OF PROPERTY
 52.0 ACRES

FEMA FLOOD ZONE DESIGNATION:
 Not located with a FEMA Flood Zone



Change of Zone Site Plan Menifee North SP260-A4

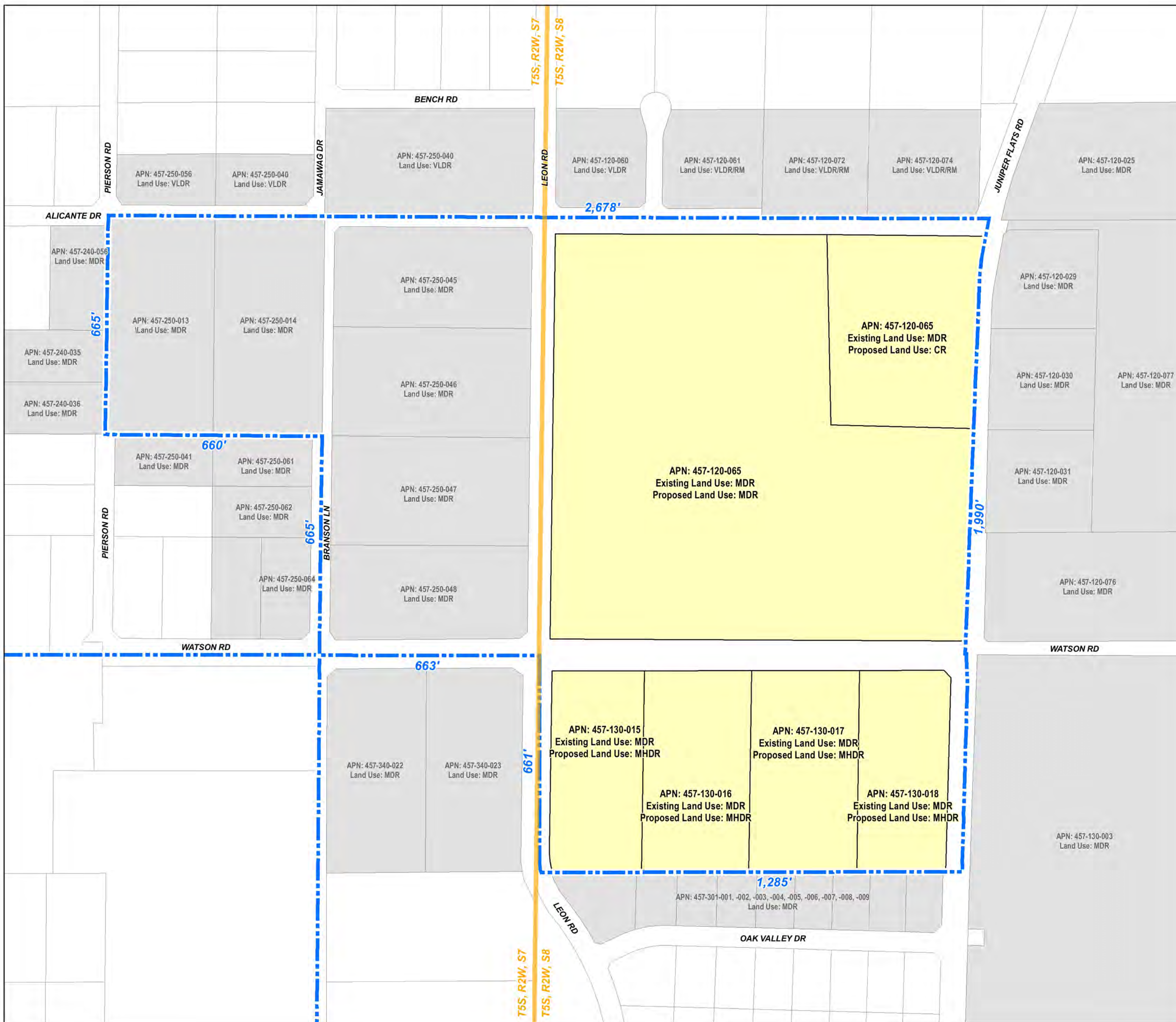
PREPARED BY:

T&B PLANNING
 3200 El Camino Real, Suite 100 Irvine, CA 92602
 p. (714) 505-6360
 www.tbplanning.com



Riverside County

JN: 471-028
 DATE: 07-07-2022



APPLICANT:
 Richland Communities
 Attn: Brian Hardy
 3161 Michelson Drive, Suite 425
 Irvine, CA 92612

LAND OWNER:
 Richland Communities
 Attn: Brian Hardy
 3161 Michelson Drive, Suite 425
 Irvine, CA 92612

LEGAL DESCRIPTION:
 See Attached.

EXISTING LAND USE DESIGNATION(S):
 Medium Density Residential (MDR)

PROPOSED LAND USE DESIGNATION(S):
 Medium Density Residential (MDR), Medium High Density Residential (MHDR), Commercial Retail (CR)

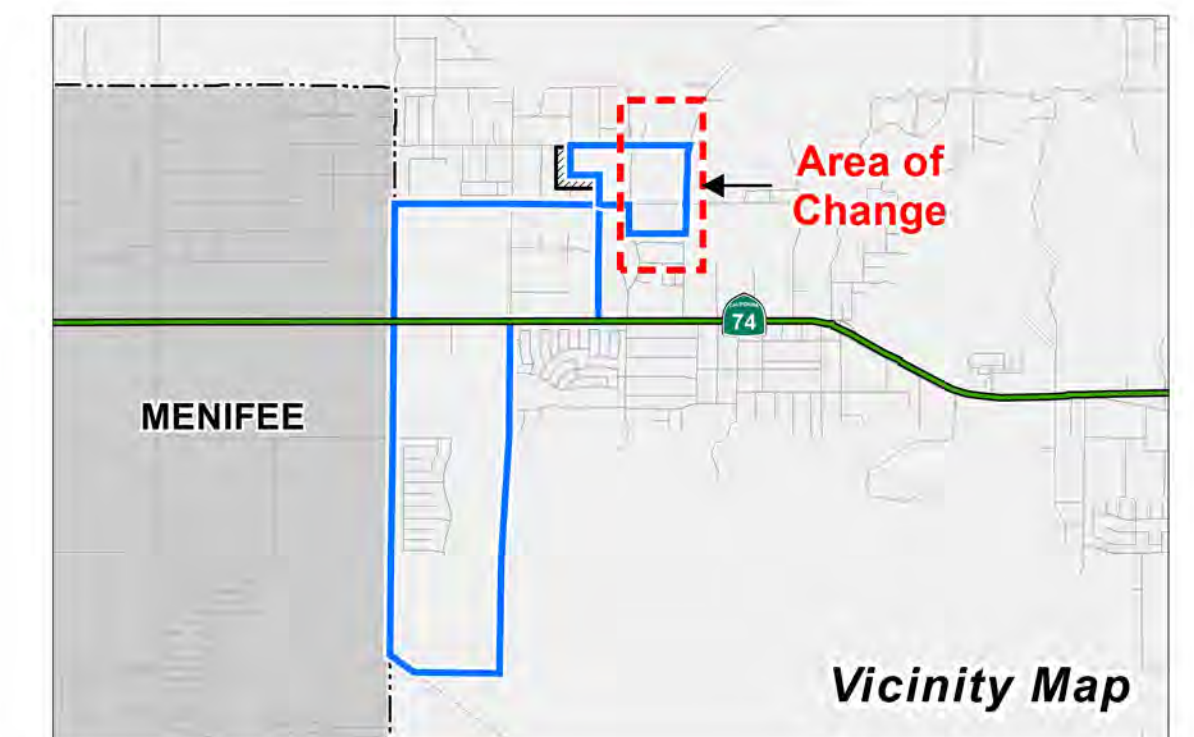
AMENDMENT DESCRIPTION:
 Amend the Harvest Valley/Winchester Area Plan from Medium Density Residential to Medium Density Residential, Medium High Density Residential, Commercial Retail

ASSESSOR'S PARCEL NUMBER(S):
 457-120-065, 457-130-015, 457-130-016, 457-130-017, 457-130-018

UTILITIES:
 Water: EMWD
 Sewer: EMWD
 Gas: So Cal Gas
 Electric: So Cal Edison
 Telephone: Frontier Communications

ACRES OF PROPERTY
 52.0 ACRES

FEMA FLOOD ZONE DESIGNATION:
 Not located with a FEMA Flood Zone



General Plan Amendment Site Plan Menifee North SP260-A4

PREPARED BY:

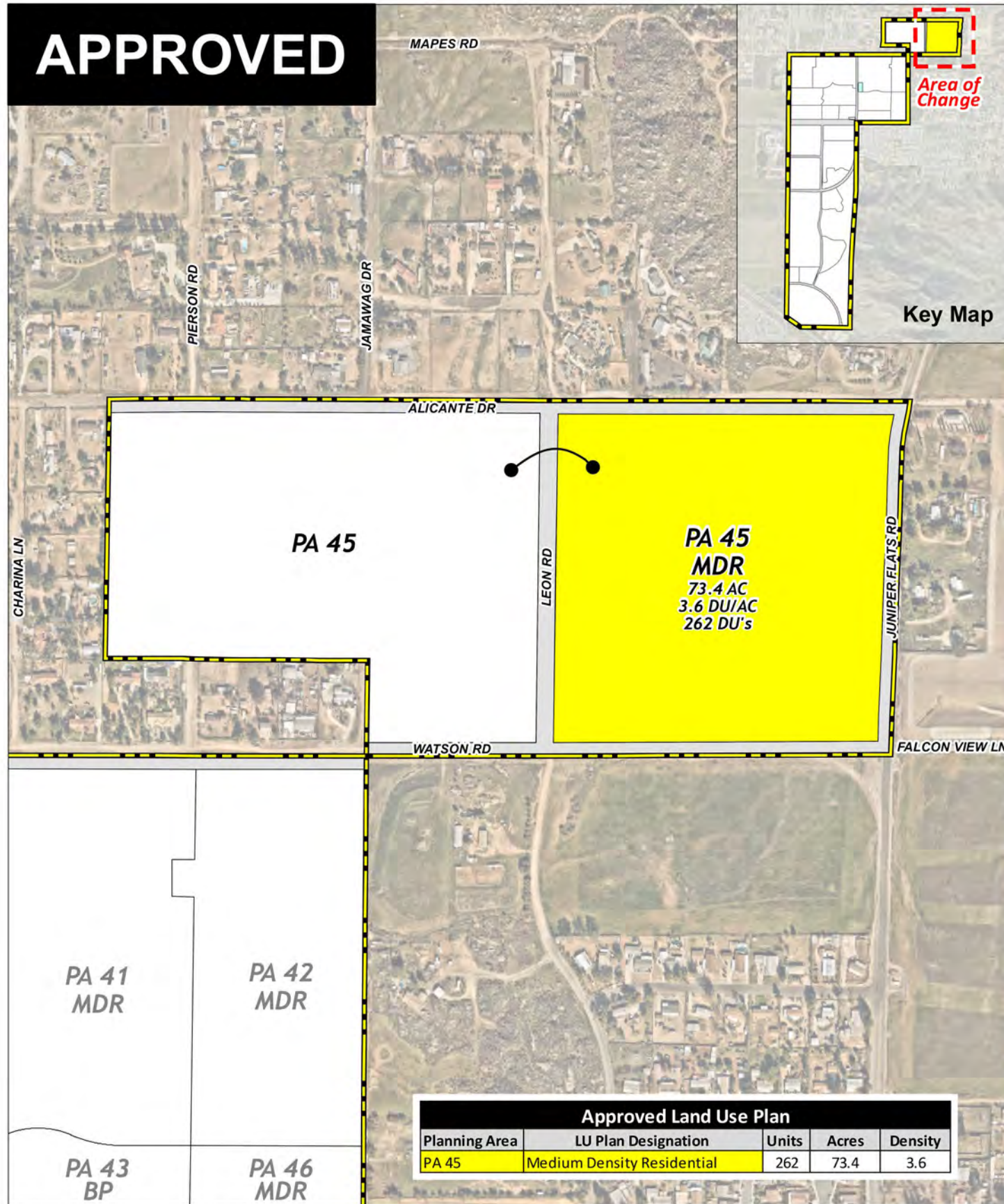
T&B PLANNING
 3200 El Camino Real, Suite 100 Irvine, CA 92602
 p. (714) 505-6360
 www.tbplanning.com



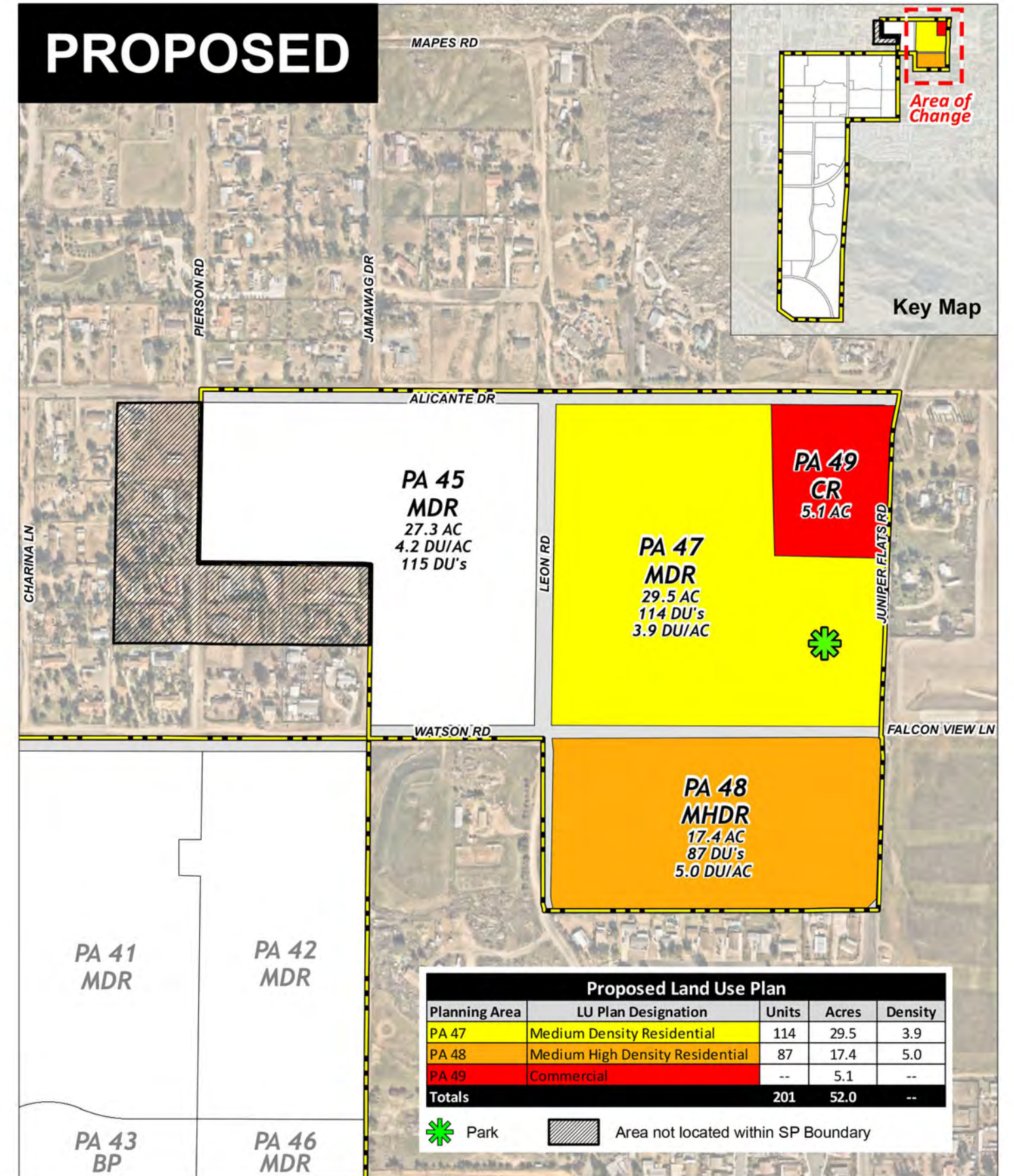
Riverside County

JN: 471-028
 DATE: 07-07-2022

APPROVED



PROPOSED



Sources: ESRI, Nearmap Aerial (2022), RCTLMA (2021)

Specific Plan Amendment Site Plan

Approved LUP vs. Proposed LUP

Menifee North SP260-A4

JN: 471-028
DATE: 07-07-2022



PLANNING AREA 48			PA 48 (CONT)			PA 47 (CONT)		
Lot Number	Lot Area (SF)	Pad Area (SF)	Lot Number	Lot Area (SF)	Pad Area (SF)	Lot Number	Lot Area (SF)	Pad Area (SF)
1	4167	3864	81	4094	4094	141	7023	4666
2	4200	3693	82	3825	3825	142	7379	4597
3	4201	3889	83	3908	3908	143	7566	4594
4	4369	4033	84	3992	3992	144	6826	4325
5	4500	4156	85	4075	4075	145	6818	4320
6	4679	4330	86	4158	4158	146	6872	5020
7	4713	4379	87	4718	4718	147	5218	4795
8	4710	4333	TOTAL	437082	422372	148	5380	4910
9	7893	7099	LOT 204	12220	--	149	7443	5048
10	11224	10342	LOT 205	12377	--	150	5030	4668
11	6057	6057	LOT 206	10379	--	151	5148	4782
12	3917	3917	LOT 207	2593	--	152	5151	4695
13	4650	4650	LOT 208	119043	--	153	4456	4456
14	3999	3999	LOT 209	1548	--	154	4822	4822
15	3999	3999	LOT 210	3613	--	155	4391	4391
16	3999	3999	LOT 211	1276	--	156	4401	4401
17	4575	4575	LOT 212	1375	--	157	4370	4370
18	6773	6773	TOTAL	164424	--	158	4373	4337
19	4470	4470	PA 48 RESIDENTIAL LOT SUMMARY			159	4373	4373
20	4928	4928	AVERAGE	5024	4855	160	4373	4373
21	5812	5812	MINIMUM	3780	3780	161	4373	4373
22	6400	6400	MAXIMUM	11224	10342	162	4373	4373
23	6593	6593				163	4373	4373
24	4832	4556				164	4883	4883
25	4047	3780				165	4887	4887
26	4391	3989				166	4410	4410
27	5556	5212				167	4500	4500
28	4387	4387				168	4500	4500
29	5005	4593				169	4500	4500
30	4441	4396				170	4500	4500
31	5148	4813				171	4698	4698
32	5795	5432				172	4500	4500
33	5400	4856				173	4500	4500
34	7383	6031				174	4500	4500
35	8242	6686				175	4500	4500
36	7894	7894				176	4634	4634
37	8117	8117				177	4848	4544
38	6862	6862				178	4974	4683
39	4085	4085				179	4945	4685
40	4028	4028				180	4916	4687
41	3870	3870				181	4887	4887
42	4352	4352				182	4891	4891
43	4512	4512				183	4590	4590
44	4155	4155				184	4590	4590
45	4920	4505				185	4590	4590
46	4492	4095				186	4590	4590
47	4601	4127				187	4590	4590
48	4953	4398				188	4742	4742
49	4776	4312				189	5175	4930
50	5184	4561				190	4845	4845
51	11023	9475				191	4855	4855
52	7048	7048				192	4864	4864
53	6789	6789				193	4874	4874
54	7019	7019				194	4880	4880
55	4094	4094				195	5029	5029
56	3780	3780				196	4582	4582
57	3780	3780				197	4590	4590
58	3780	3780				198	4590	4590
59	3780	3780				199	4590	4590
60	4134	4134				200	4612	4612
61	4300	4300				TOTAL	632528	548034
62	3825	3825				LOT 201	183352	--
63	3908	3908				LOT 203	99274	--
64	3990	3990				LOT 213	587	--
65	4070	4070				LOT 214	5320	--
66	3915	3915				LOT 215	891	--
67	7804	7804				LOT 216	1041	--
68	3821	3821				LOT 217	677	--
69	3780	3780				LOT 218	1070	--
70	3780	3780				LOT 219	989	--
71	3780	3780				LOT 220	468	--
72	3818	3818				LOT 221	14953	--
73	3787	3787				LOT 222	17498	--
74	3942	3942				LOT 223	729	--
75	4045	4045				LOT 224	5819	--
76	5086	5086				LOT 225	401	--
77	8062	8062				LOT 226	35749	--
78	6408	6408				LOT 227	38247	--
79	4589	4589				TOTAL	407494	--
80	4119	4119						

LEGAL DESCRIPTION:
 PARCEL A: (APN: 457-120-065)
 PARCEL 5 OF PARCEL MAP NO. 9021, IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS SHOWN BY MAP ON FILE IN BOOK 40, PAGES 13 TO 15 OF PARCEL MAPS, RIVERSIDE COUNTY RECORDS.
 PARCEL B: (APN: 457-130-015) (PARCEL 7), 457-130-016 (PARCEL 2), 457-130-017 (PARCEL 3), 457-130-018 (PARCEL 4), AND LOTS B, C, D, AND E (APN'S NOT YET ASSESSED).
 PARCELS 1, 2, 3, 4 AND LETTERED LOTS B, C, D, AND E OF PARCEL MAP NO. 1287, IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 69, PAGE 1 OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

BENCHMARK:
 BENCH MARK: A STANDARD U.S.C. & G.S. BRASS DISK TOP LARGE BOULDER AS DESCRIBED BY COAST AND GEODETIC SURVEY 1935 17 MILES NORTH ALONG JUNIPER FLATS ROAD; FROM THE INTERSECTION OF JUNIPER FLATS AND HIGHWAY 74 TO A DIRT ROAD (NO NAME), THEN 0.1 MILE WEST ALONG THE DIRT ROAD; 18 FEET SOUTH OF THE ROAD, A BRASS DISK SET IN THE TOP OF THE MIDDLE ONE OF THREE LARGE BOULDERS; AND MARKED S-311 U.S.C. & G.S. 1935; 1 FOOT ABOVE GROUND. COUNTY OF RIVERSIDE BENCHMARK I.D. W-108 & S-311 ELEVATION: 1722.499 (NGVD_29)

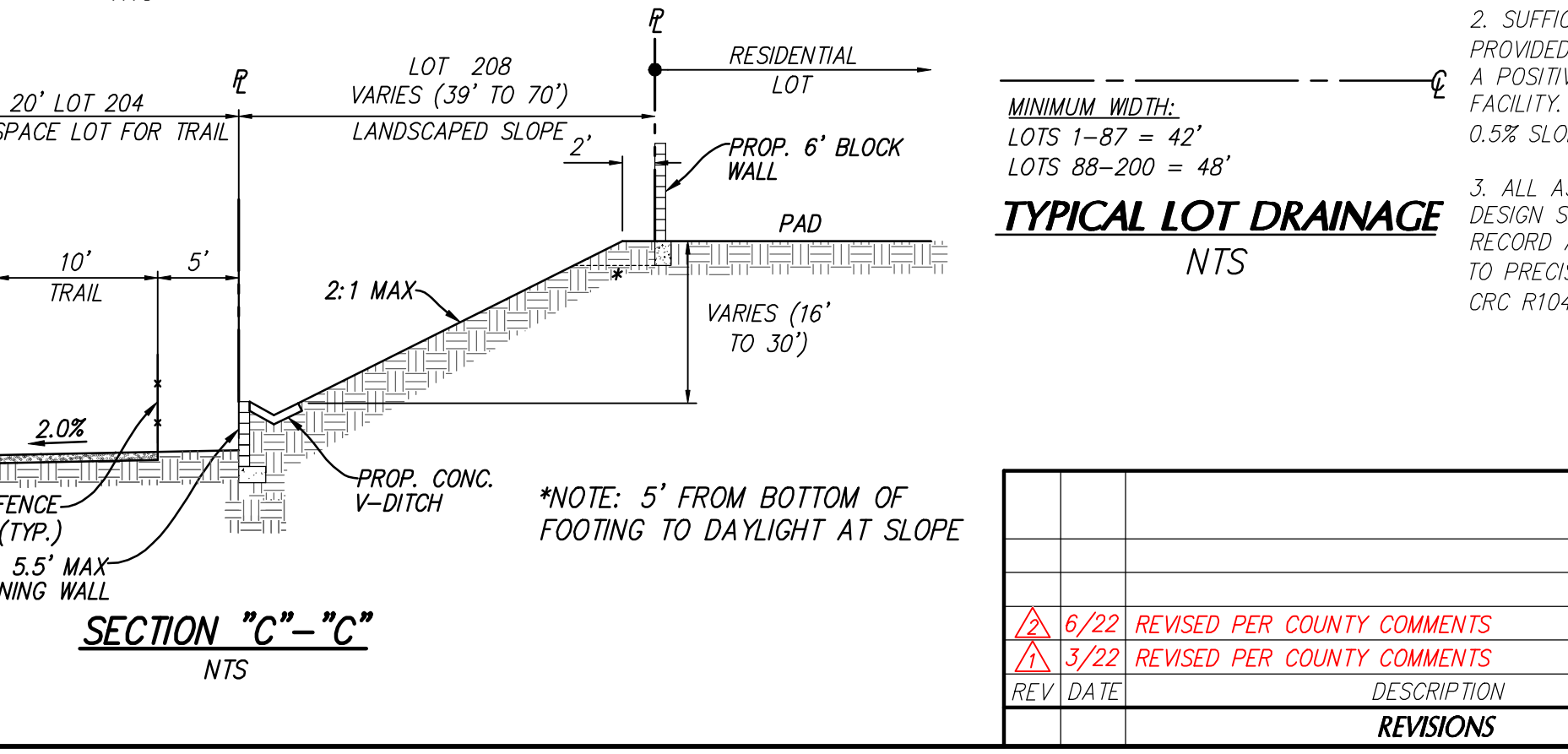
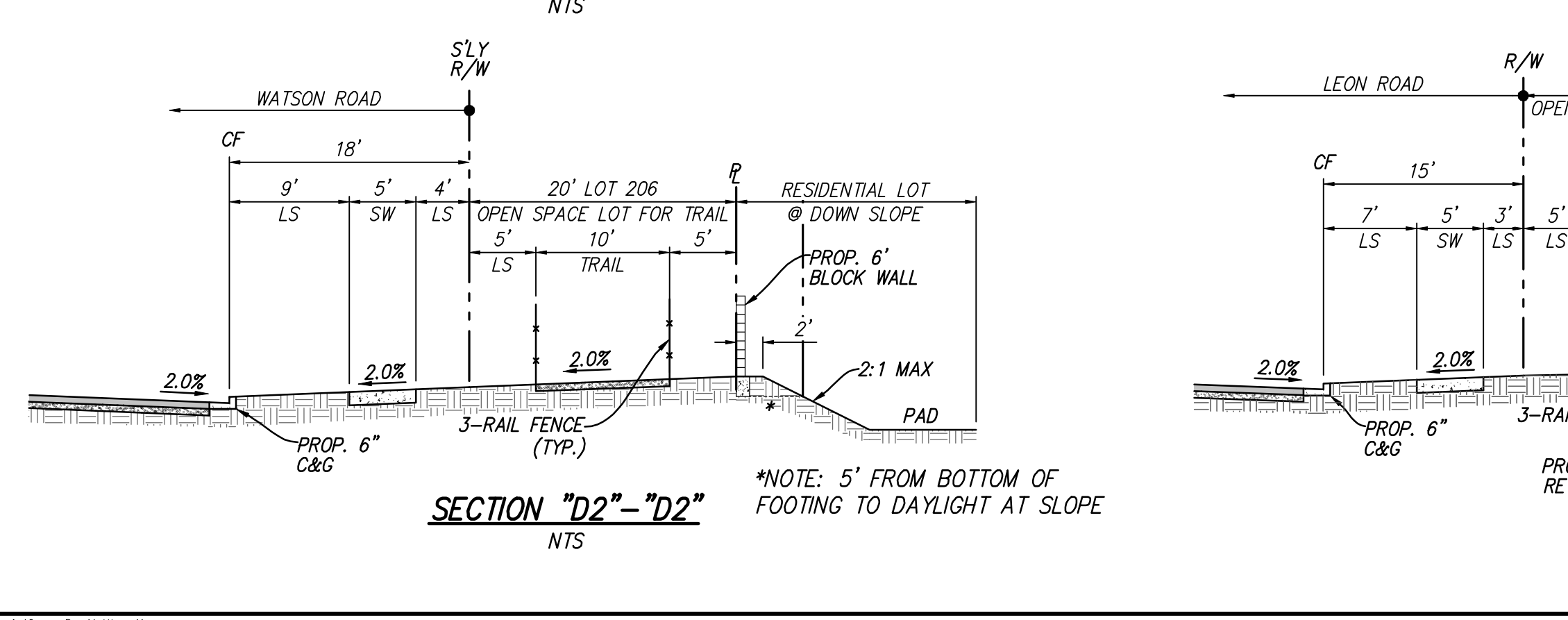
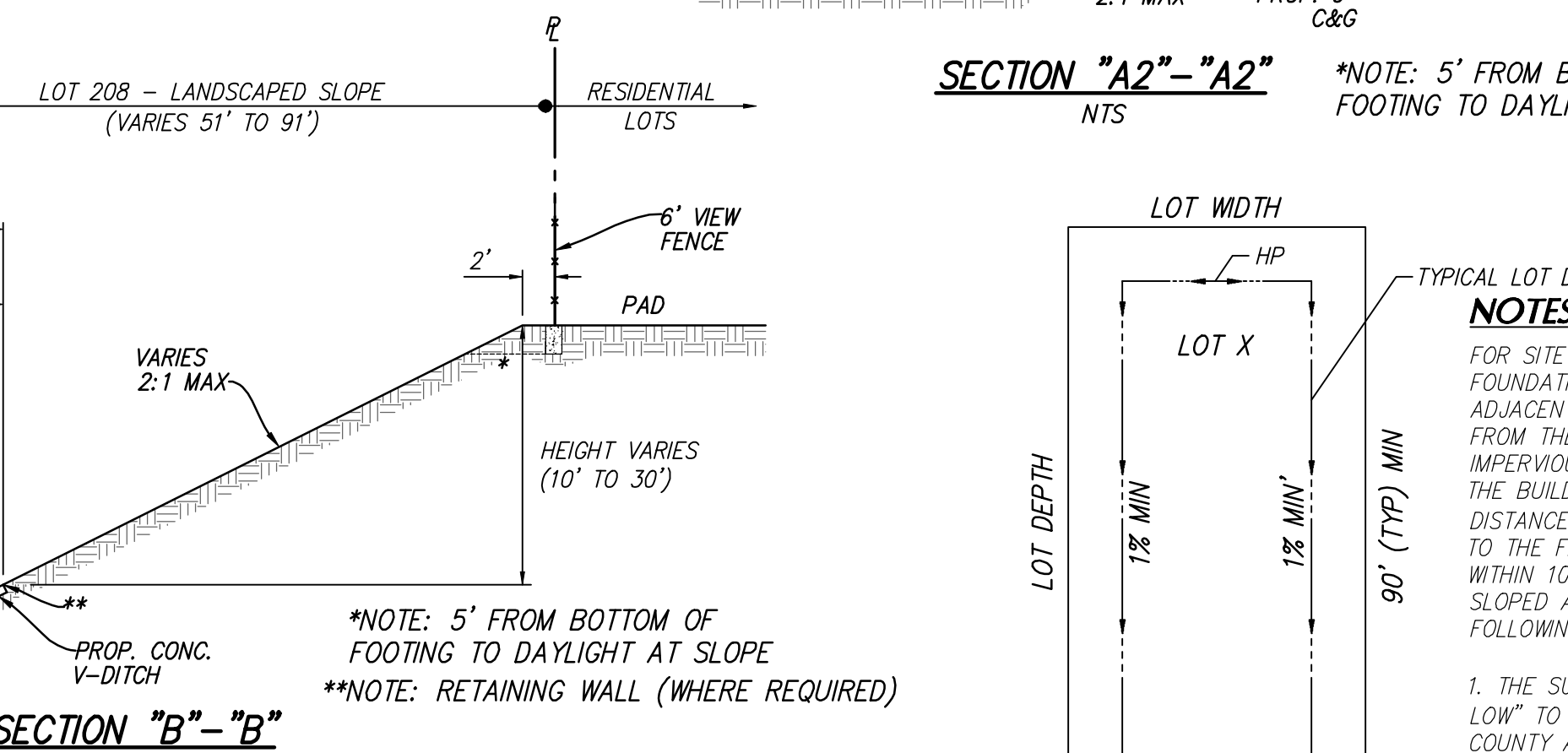
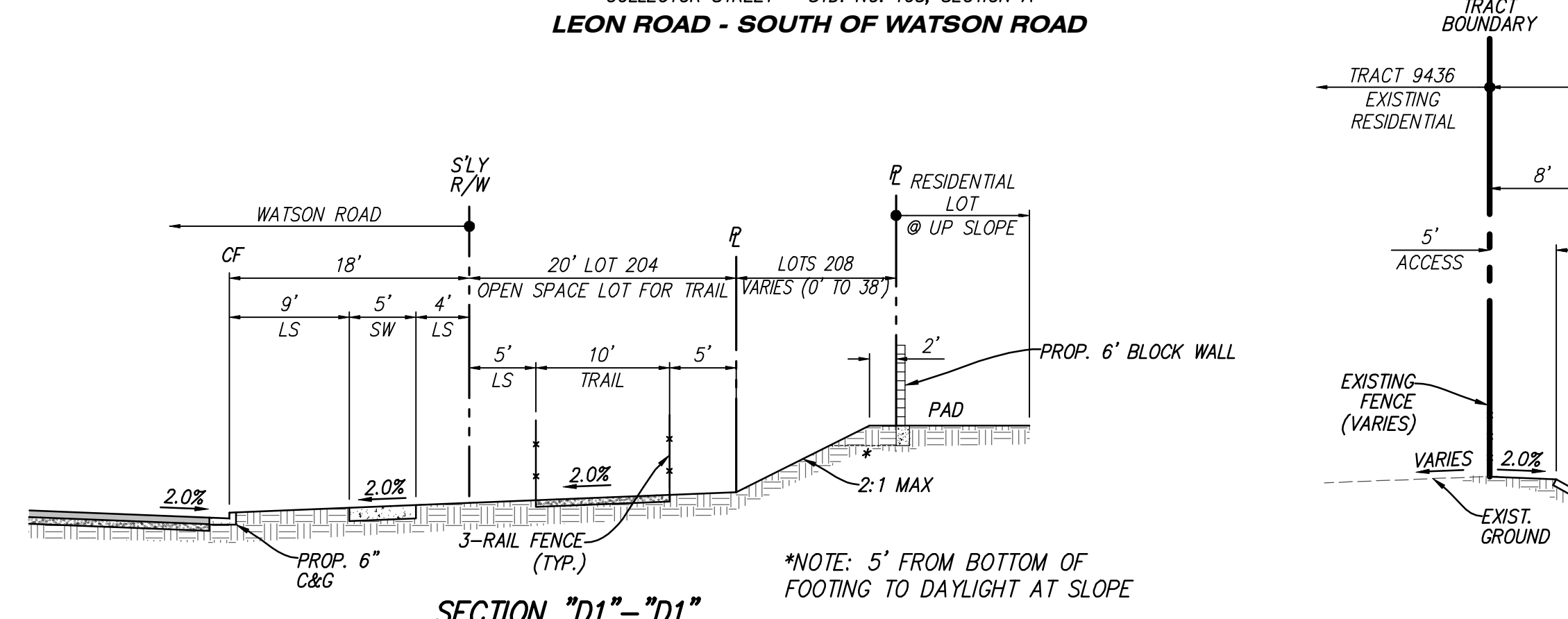
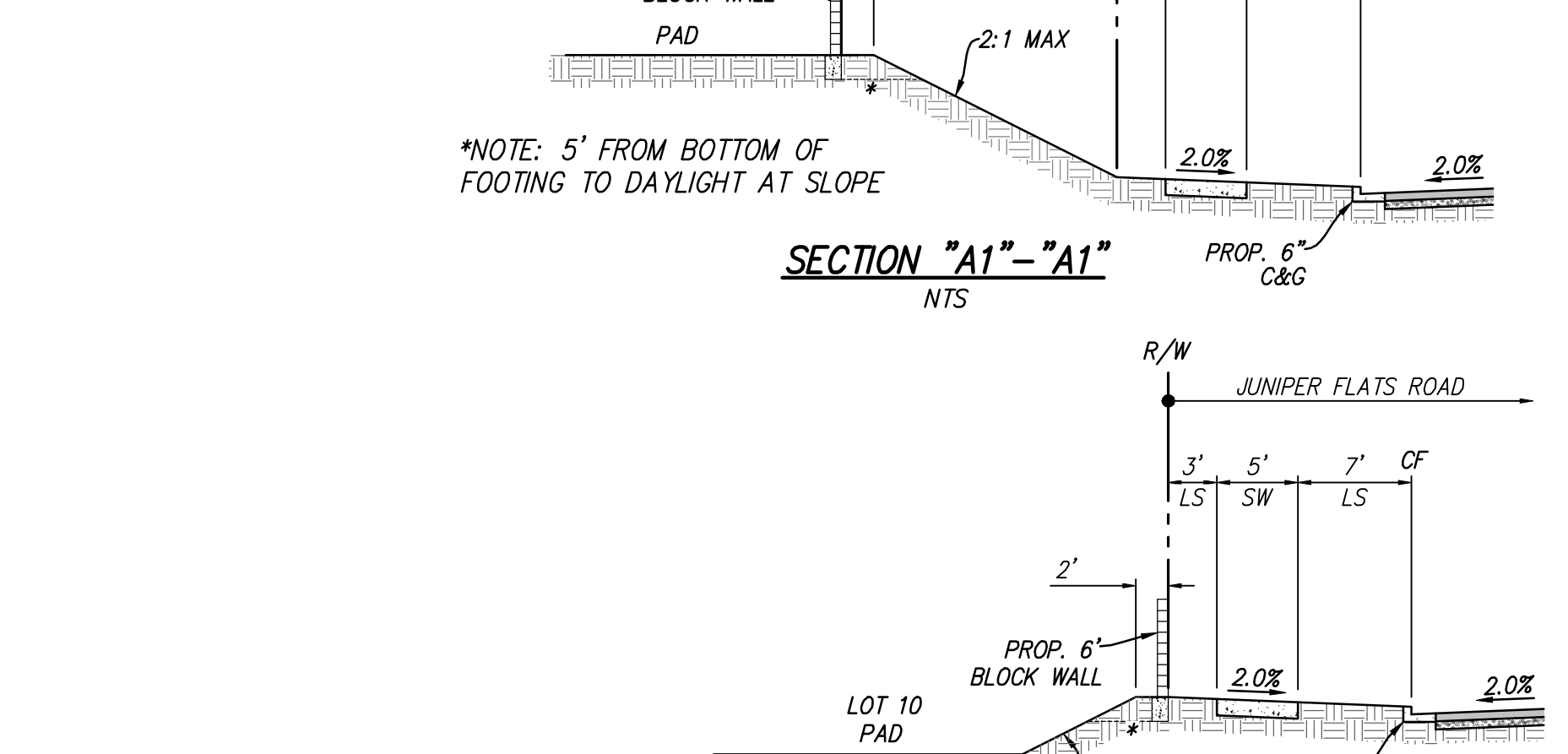
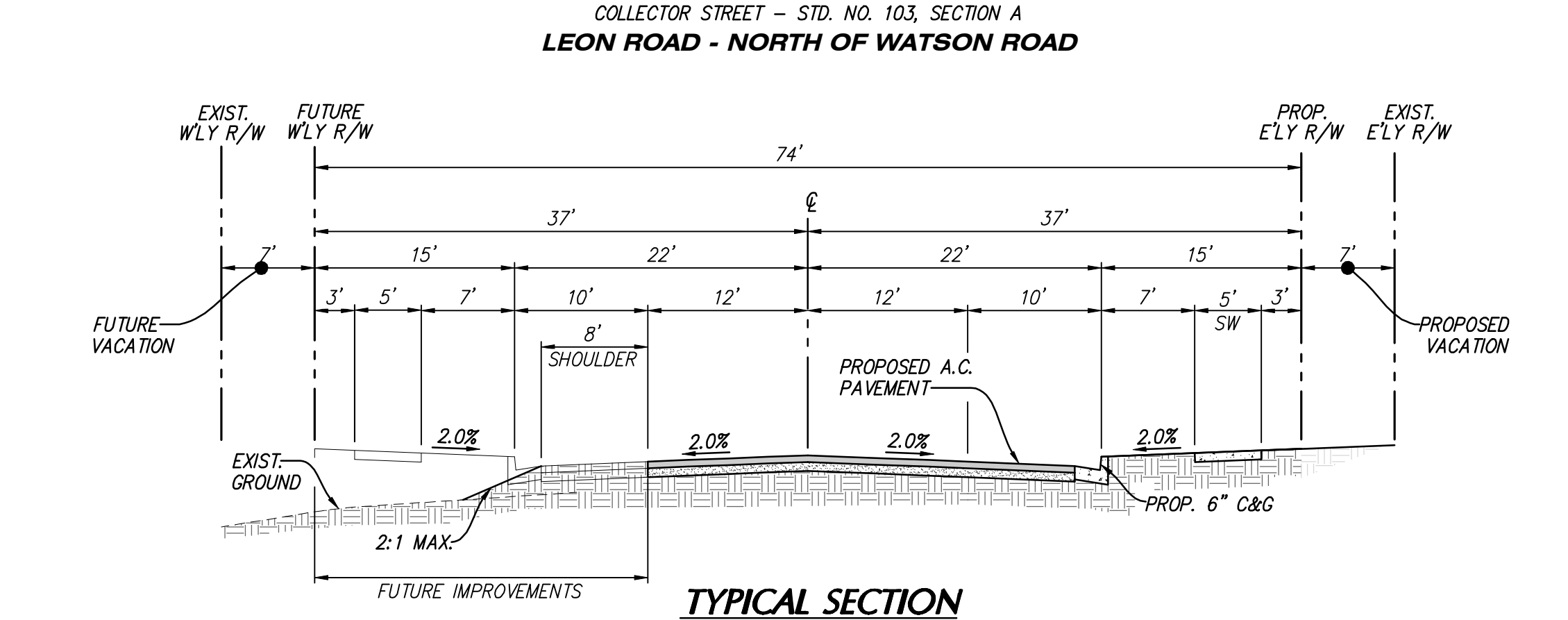
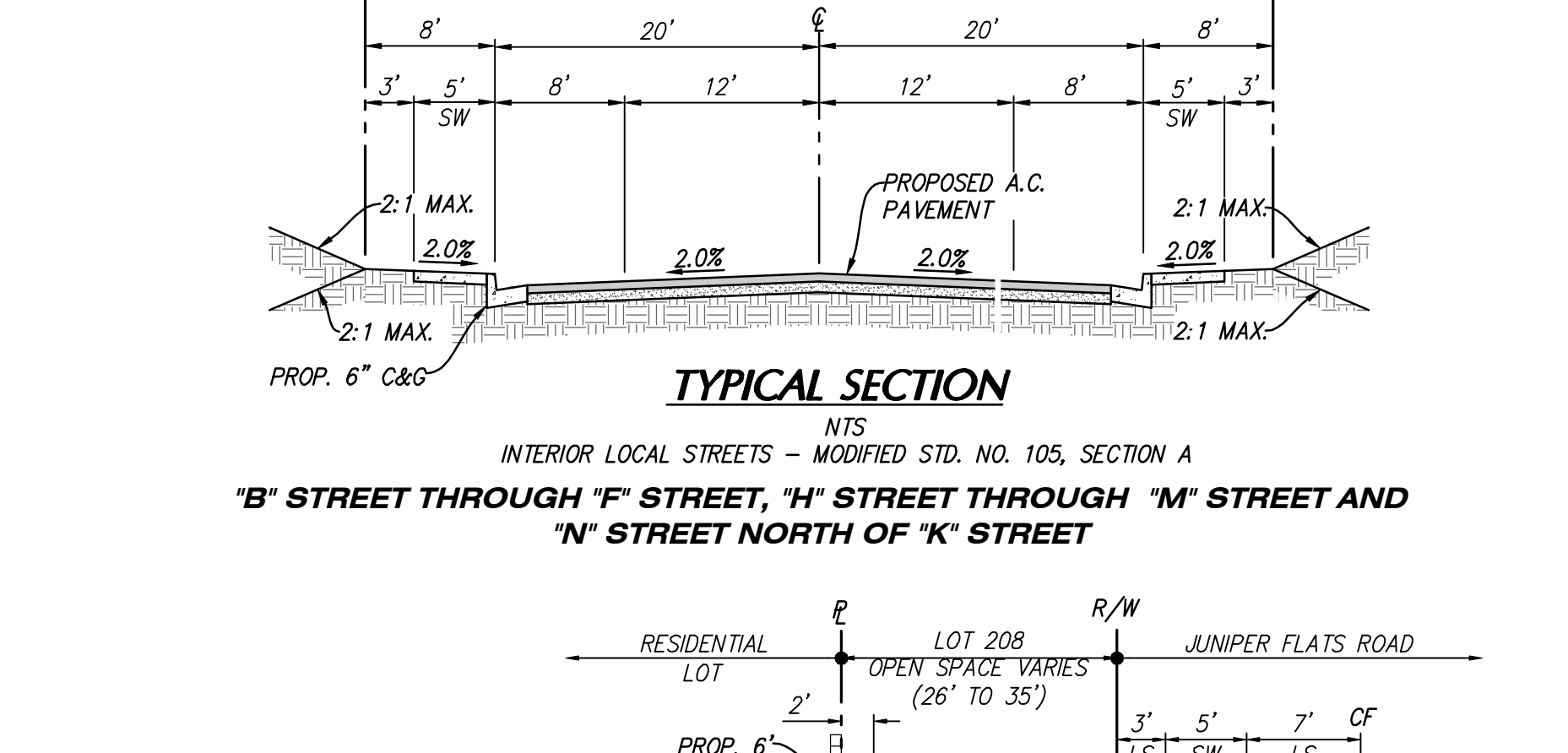
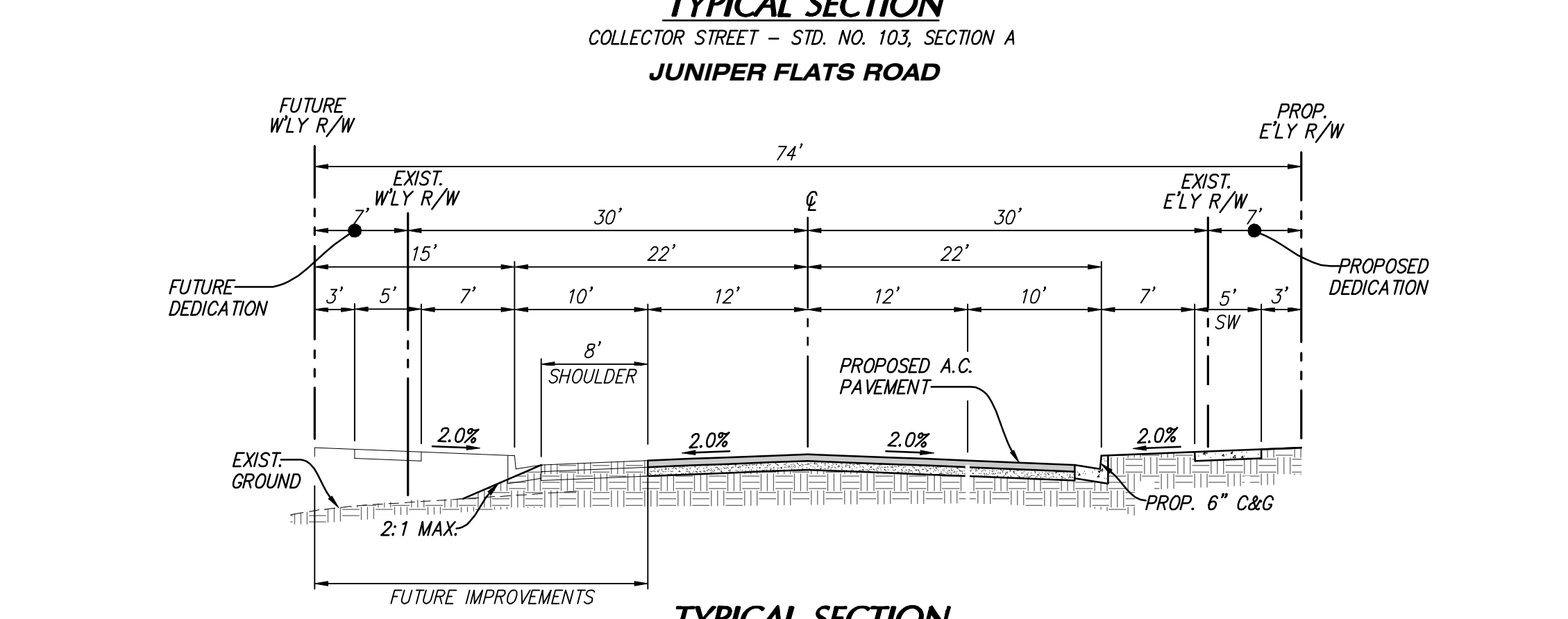
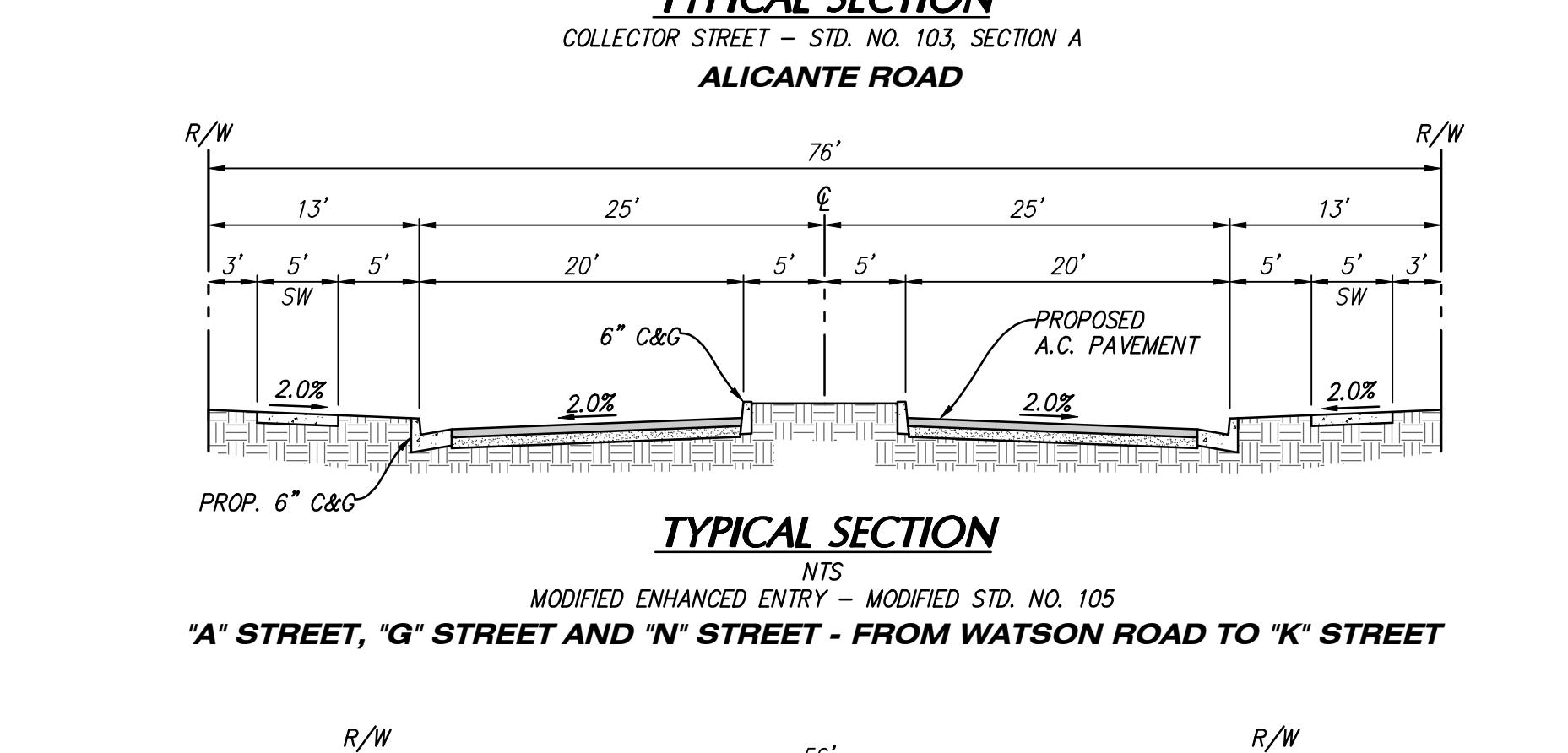
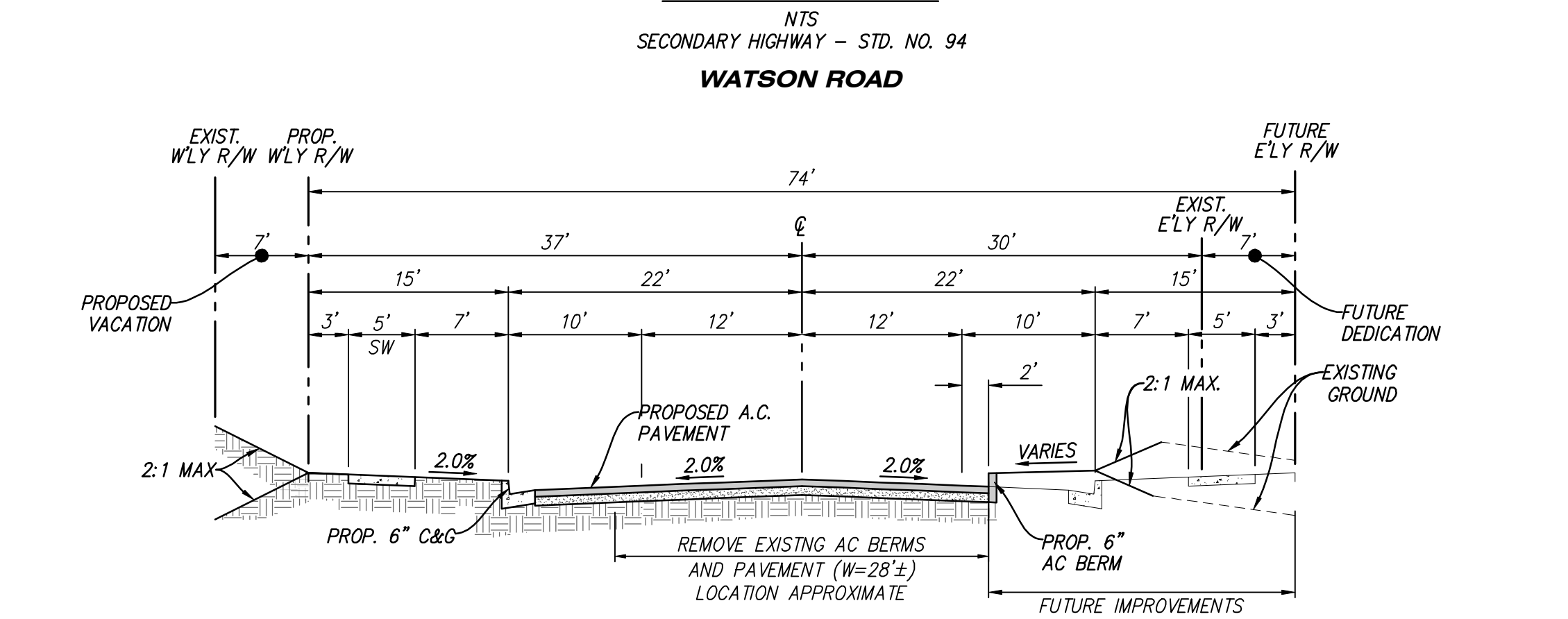
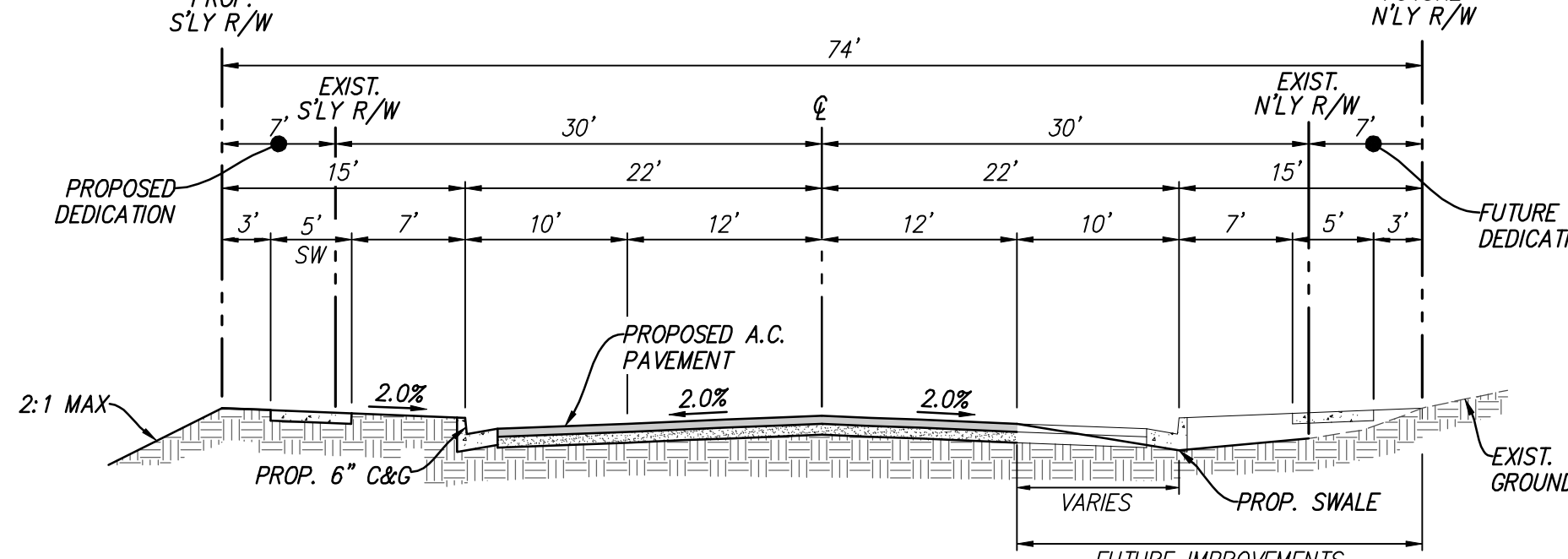
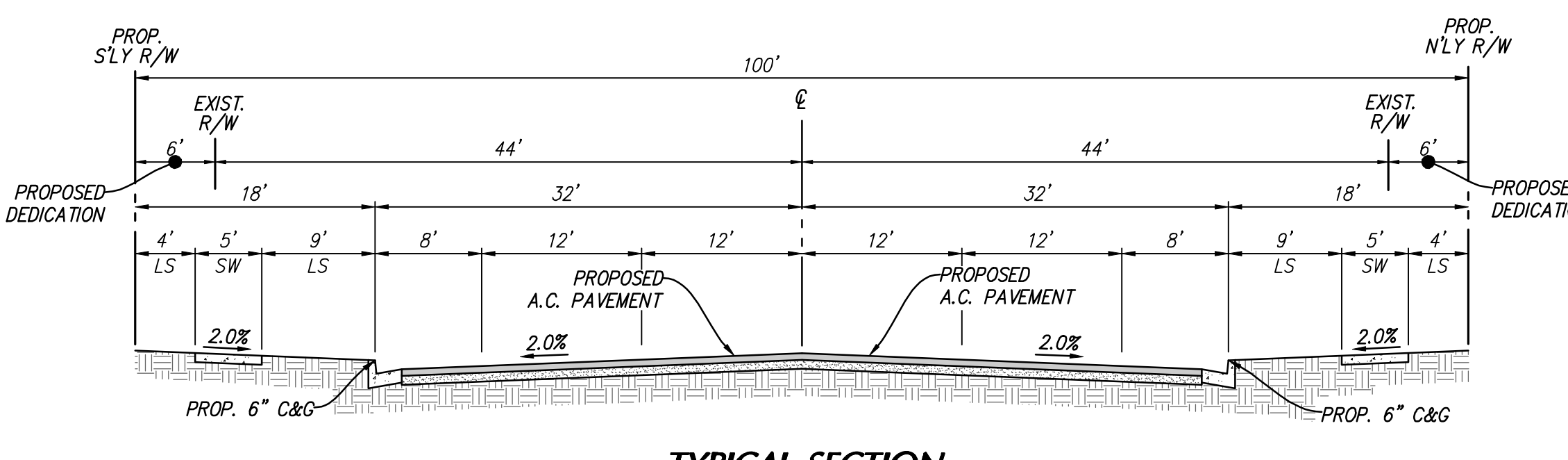
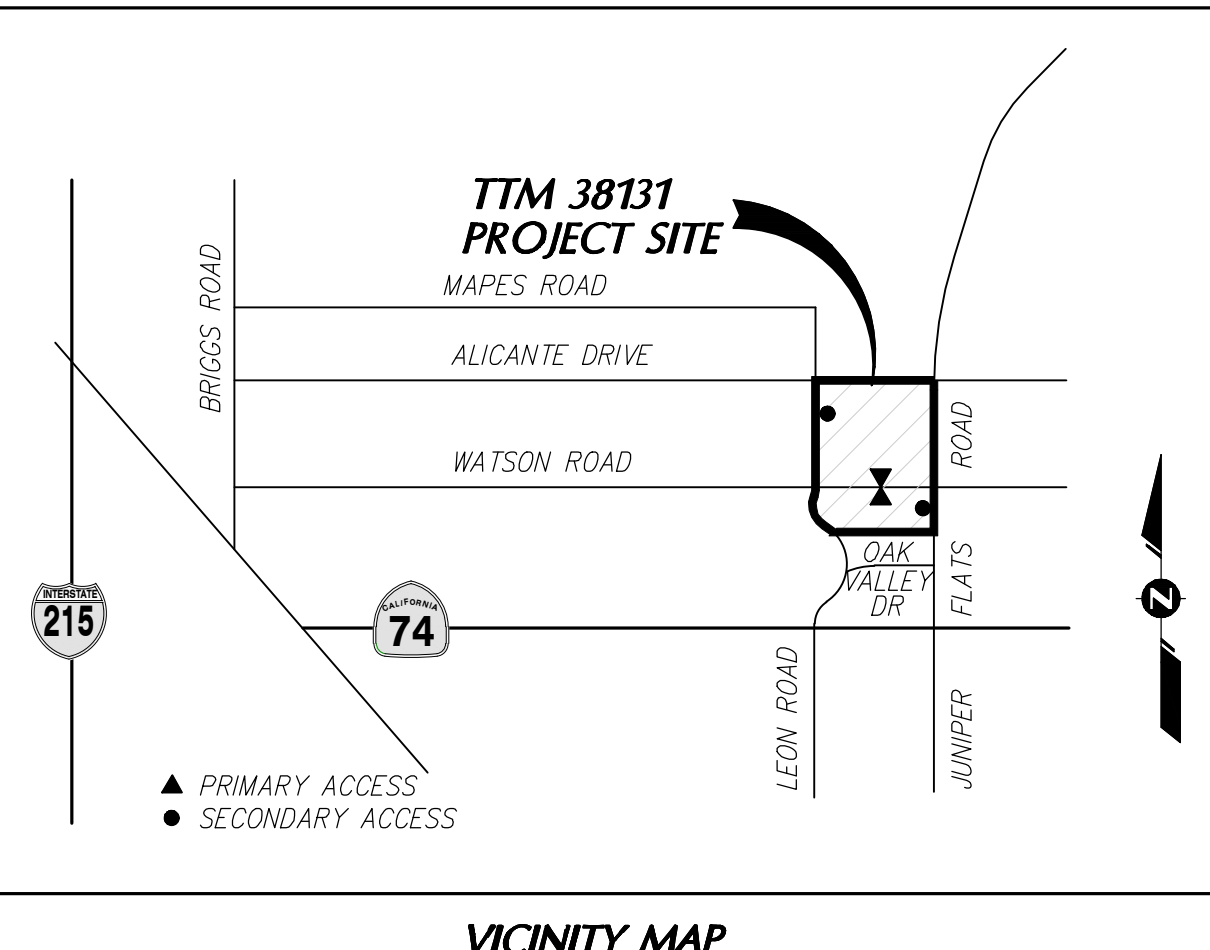
IN THE COUNTY OF RIVERSIDE TENTATIVE TRACT MAP NO. 38131

OWNER/APPLICANT
 HOW LLC CENTER LLC
 JHB COLONY INVESTMENTS, LLC
 3161 MICHELSON DR., SUITE 425
 IRVINE, CA 92612
 ATTN: BRAD HARRY
 PH: 949-698-2191

ENGINEER
 K & A ENGINEERING, INC.
 357 N. SHERIDAN STREET, SUITE 117
 CORONA, CA 92780
 PHONE: (951) 279-1800
 PROJECT MANAGER: KEITH OSBORN, P.E.

UTILITIES AND SERVICES
 AGENCY SERVING THE AREA
 ELECTRIC: SOUTHERN CALIFORNIA EDISON
 GAS: SOUTHERN CALIFORNIA GAS CO.
 TELEPHONE: VERIZON
 CABLE TV: FRONTIER/SPECTRUM
 WATER: EASTERN MUNICIPAL WATER DISTRICT
 SEWER: EASTERN MUNICIPAL WATER DISTRICT
 SCHOOL DIST: MENEFEE UNIFIED SCHOOL DISTRICT, ROMOLAND & PERRIS UNION HIGH SCHOOL

SOILS ENGINEER
 LCC GEOTECHNICAL, INC.
 131 CALLE IGLESIA, SUITE 200
 SAN CLEMENTE, CA 92672
 ATTN: DENNIS BORATINEG, PE, GE
 PH: 949-369-6141



PROPERTY ACREAGE
 GROSS = 59.6 AC.
 NET = 52.4 AC.

ASSESSOR'S PARCEL NUMBER
 457-120-065, 457-130-015, 457-130-016, 457-130-017
 AND 457-130-018

SECTIONS, TOWNSHIPS AND RANGES
 PORTION OF T5S, R2W, SECTION 8

PRESENT USE
 VACANT

THOMAS BROTHERS MAP
 PORTIONS OF PAGE 809-GRID B7

CURRENT GENERAL PLAN DESIGNATION
 MEDIUM DENSITY RESIDENTIAL

PROPOSED USE
 RESIDENTIAL, COMMERCIAL, WATER QUALITY BASIN AND PUBLIC PARK

EXISTING ZONING
 NORTH OF WATSON - SP 260 - PA 45 -MDR
 SOUTH OF WATSON - RURAL RESIDENTIAL

PROPOSED ZONING

PLANNING AREA	PROPOSED ZONING PER	PROPOSED LAND USE
47	SP 260 - A4	MEDIUM DENSITY RESIDENTIAL (MDR)
48	SP 260	MEDIUM DENSITY RESIDENTIAL (MDR)
49	SP 260	COMMERCIAL (CR)

COMMON AREA SUMMARY

PLANNING AREA	LOT NO.	PROPOSED USE
47	203	WO & DETENTION BASIN
48	204-209	OPEN SPACE/SLOPES
48	210	OPEN SPACE, UTILITIES & EMERGENCY ACCESS
48	211-212	OPEN SPACE/SLOPES
47	213-226	OPEN SPACE/SLOPES
47	227	PARK SITE

PROPOSED DEVELOPMENT

PLANNING AREA	LAND USE	NO. OF LOTS/ACREAGE
47	MDR	113/20.2 AC. 13/5.1 AC. 4.2 AC.
48	MHDR	87/13.7 AC. 9/3.8 AC. --
49	CR	-- -- -- 5.4 AC. 5.4 AC.

TOTAL AREA = 52.4 AC.

* AREA INCLUDES STREET RIGHT OF WAY

STREET SUMMARY

STREET	LENGTH (LF)	"E" STREET -	LENGTH (LF)
WATSON ROAD	1290	"E" STREET -	351
JUNIPER FLATS ROAD	2007	"G" STREET -	181
LEON ROAD	2008	"H" STREET -	193
ALICANTE ROAD	1335	"I" STREET -	478
"A" STREET -	198	"J" STREET -	483
"B" STREET -	895	"K" STREET -	474
"C" STREET -	346	"L" STREET -	694
"M" STREET -	283	"N" STREET -	677
"E" STREET -	298	"N" STREET -	1092

ABBREVIATIONS

A.C. ASPHALT CONCRETE
 BVC BEGINNING OF VERTICAL CURVE
 C&G CURB & GUTTER
 CL, E CENTERLINE
 DWY DRIVEWAY
 ELY EASTERLY
 ESA ENVIRONMENTALLY SENSITIVE AREA
 EVC END OF VERTICAL CURVE
 EX, EXIST EXISTING
 FG FINISH GRADE
 HOA HOMEOWNER'S ASSOCIATION
 MDP MASTER DRAINAGE PLAN
 MIN MINIMUM
 MAX MAXIMUM
 P PAD
 P, P/L PROPERTY LINE
 PM PARCEL MAP
 PROP PROPOSED
 R/W, RW RIGHT-OF-WAY
 SW SOUTHERLY
 SLY SOUTHWEST
 TR BDY TRACT BOUNDARY
 TRW TOP OF RETAINING WALL
 WLY WESTERLY

LEGEND

TRACT BOUNDARY
 PROPERTY LINE
 PROP. R/W DEDICATION
 EXISTING EASEMENT
 EXISTING CONTOUR
 PROPOSED CONTOUR
 LOT NUMBER
 LETTERED LOT
 PROPOSED PAD ELEVATION
 EXISTING POWER POLE
 PROPOSED STORM DRAIN
 EXISTING STORM DRAIN
 PROPOSED DOMESTIC WATER
 EXISTING DOMESTIC WATER
 PROPOSED SEWER
 PROP. RETAINING WALL

PROPOSED IMPROVEMENT SCHEDULE
 SCHEDULE "A"

SHEET INDEX

SHEET NO.	DESCRIPTION
1	TITLE SHEET, STREET SECTIONS & NOTES
2	TENTATIVE TRACT MAP
3	PHASING MAP & SECTIONS

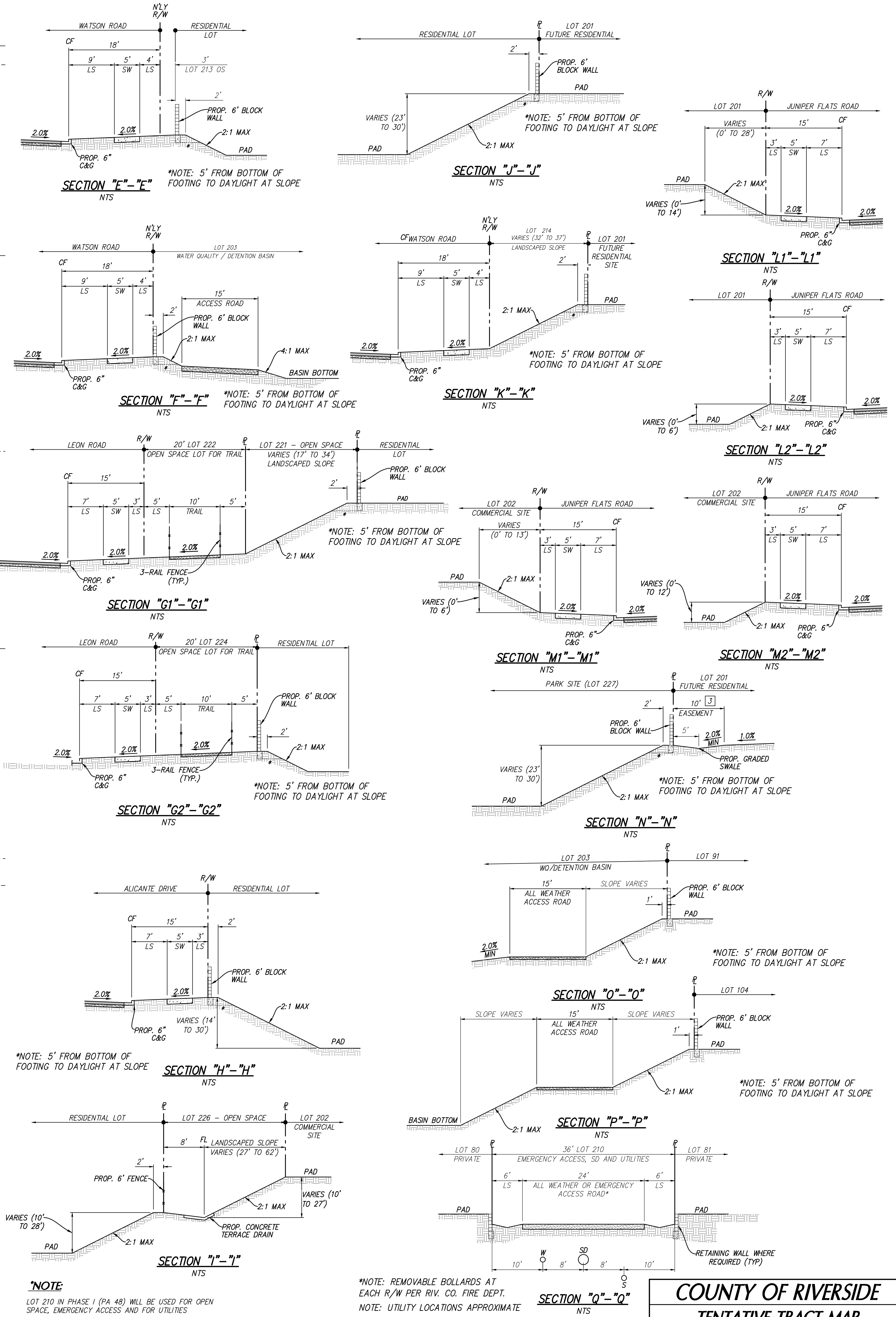
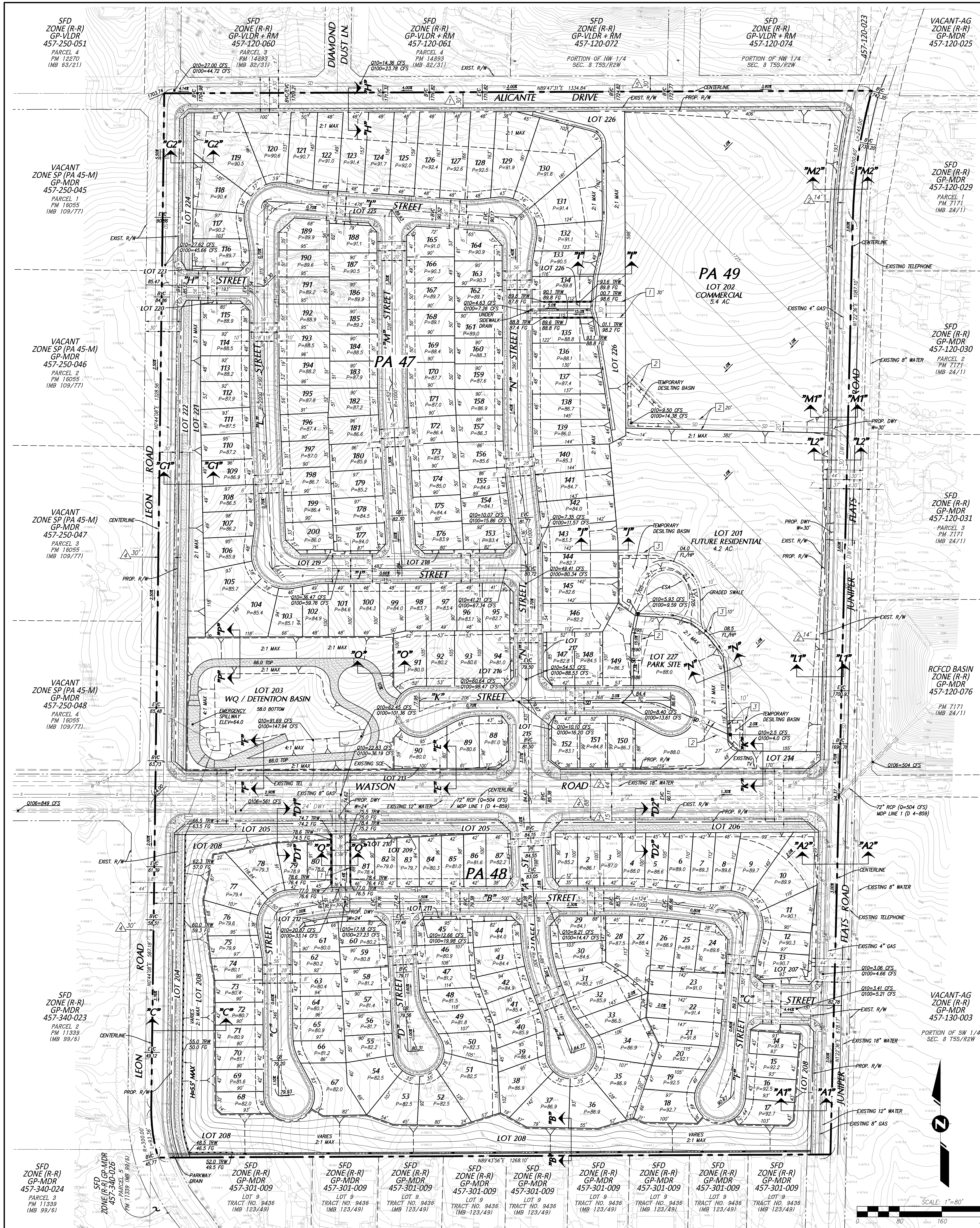
COUNTY OF RIVERSIDE
TENTATIVE TRACT MAP NO. 38131
SCHEDULE "A"
 TITLE SHEET, STREET SECTIONS & NOTES

Drawn By: PREPARED: AUGUST 2021 DWG. NO. 1
 Checked By: SHEET
 Scale: AS SHOWN OF 3 SHEETS

REVISIONS

NO.	DATE	DESCRIPTION	APPROVED
6/22		REVISED PER COUNTY COMMENTS	
3/22		REVISED PER COUNTY COMMENTS	

IN THE COUNTY OF RIVERSIDE TENTATIVE TRACT MAP NO. 38131



SFD ZONE (R-R) GP-VLDR 457-250-051 PARCEL 4 PM 12070 (MB 65/21)
 SFD ZONE (R-R) GP-VLDR + RM 457-120-060 PARCEL 3 PM 14393 (MB 82/31)
 SFD ZONE (R-R) GP-VLDR + RM 457-120-061 PARCEL 4 PM 14393 (MB 82/31)
 SFD ZONE (R-R) GP-VLDR + RM 457-120-072 PARCEL 4 PM 14393 (MB 82/31)
 VACANT-AG ZONE (R-R) GP-MDR 457-120-025 PARCEL 1 PM 18055 (MB 109/77)
 SFD ZONE (R-R) GP-MDR 457-120-029 PARCEL 1 PM 17171 (MB 24/11)
 SFD ZONE (R-R) GP-MDR 457-120-030 PARCEL 2 PM 17171 (MB 24/11)
 SFD ZONE (R-R) GP-MDR 457-120-031 PARCEL 3 PM 17171 (MB 24/11)
 RCFCD BASIN ZONE (R-R) GP-MDR 457-120-076 PM 7171 (MB 24/11)
 SFD ZONE (R-R) GP-MDR 457-340-023 PARCEL 2 PM 11339 (MB 99/6)
 SFD ZONE (R-R) GP-MDR 457-340-024 PARCEL 3 PM 11339 (MB 99/6)
 SFD ZONE (R-R) GP-MDR 457-301-009 TRACT NO. 9436 (MB 123/49)
 SFD ZONE (R-R) GP-MDR 457-301-009 TRACT NO. 9436 (MB 123/49)
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 SFD ZONE (R-R) GP-MDR 457-301-009 TRACT NO. 9436 (MB 123/49)
 SFD ZONE (R-R) GP-MDR 457-301-009 TRACT NO. 9436 (MB 123/49)

NOTE:
 LOT 210 IN PHASE 1 (PA 48) WILL BE USED FOR OPEN SPACE, EMERGENCY ACCESS AND FOR UTILITIES
 NOTE: UTILITY LOCATIONS APPROXIMATE

REV	DATE	DESCRIPTION	APPROVED
6/22		REVISED PER COUNTY COMMENTS	
3/22		REVISED PER COUNTY COMMENTS	

KA ENGINEERING & SURVEYING
 357 N. SHERRIDAN STREET
 SUITE 117
 CORONA, CALIFORNIA 92708
 TEL. (951) 279-1800
 FAX (951) 279-4380

COUNTY OF RIVERSIDE
 TENTATIVE TRACT MAP
 NO. 38131
 SCHEDULE "A"
 TENTATIVE TRACT MAP

Drawn By: _____ PREPARED: AUGUST 2021 DWG. NO. 2
 Checked By: _____ SHEET
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IN THE COUNTY OF RIVERSIDE
TENTATIVE TRACT MAP NO. 38131



PHASING SUMMARY

PHASE NO	PLANNING AREA	LAND USE	MIN. LOT SIZE	AVE DENSITY	NO. OF RESIDENTIAL LOTS	RESIDENTIAL LOT NOS.	NON-RESIDENTIAL LOTS
I	48	MHDR	3780 SF	5.0 DU/AC	87	1-87	LOTS 204 THRU 212
II	POR. OF 47	MDR	4320 SF	3.8 DU/AC	113	88-200	LOTS 203, 213 AND 215 THRU 227
III	49 & POR. OF 47	CR & MDR	4320 SF	N/A	1	201	LOTS 202 (COMMERCIAL) AND 214 (O.S.)

EXISTING EASEMENT NOTES

- △ COUNTY ACCEPTANCE OF ALICANTE DRIVE A 30 FOOT WIDE ROAD EASEMENT, AS SHOWN ON PARCEL MAP NO. 9021 (PMB 40/1.3 & 15). THIS DEDICATION WAS ACCEPTED FOR PUBLIC ROAD AND UTILITY USES. THIS DEDICATION WAS NOT ACCEPTED INTO THE COUNTY MAINTAINED ROAD SYSTEM, BY RIVERSIDE COUNTY RESOLUTION NO. 90-165, RECORDED APRIL 26, 1990 AS INSTRUMENT NO. 151597, OF OFFICIAL RECORDS
- △ AN EASEMENT FOR PIPELINES AND INCIDENTAL PURPOSES, RECORDED AUGUST 23, 1979 AS INSTRUMENT NO. 178499, OF OFFICIAL RECORDS IN FAVOR OF SOUTHERN CALIFORNIA GAS COMPANY, A CORPORATION
- △ AN EASEMENT FOR PIPELINES AND INCIDENTAL PURPOSES, RECORDED MAY 30, 1973 AS INSTRUMENT NO. 69780, OF OFFICIAL RECORDS IN FAVOR OF EASTERN MUNICIPAL WATER DISTRICT
- △ DEDICATION OF AN EASEMENT FOR STREET, PUBLIC UTILITIES AND INCIDENTAL PURPOSES FOR LOTS "A" THROUGH "I" AS SHOWN OF PARCEL MAP NO. 12767 (PMB 69/1). LOTS "D" THROUGH "E" WERE NOT ACCEPTED BY THE COUNTY OF RIVERSIDE
- △ EXISTING IRREVOCABLE OFFER OF DEDICATION FOR PUBLIC ROAD PURPOSES, INCLUDING PUBLIC UTILITY AND PUBLIC SERVICE USES, RECORDED AUGUST 9, 1972 AS INSTRUMENT NO. 106365

PROPOSED EASEMENT NOTES

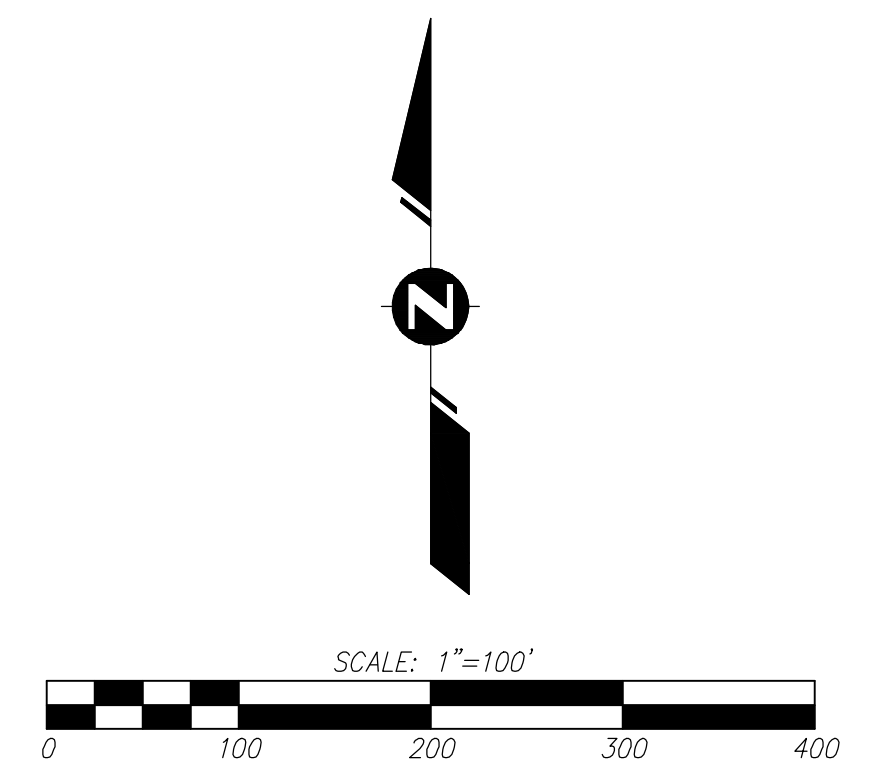
- 1 EASEMENT FOR PUBLIC STORM DRAIN, SEWER AND WATER PURPOSES DEDICATED TO THE COUNTY OF RIVERSIDE
- 2 EASEMENT FOR PUBLIC STORM DRAIN PURPOSES DEDICATED TO THE COUNTY OF RIVERSIDE
- 3 EASEMENT TO VALLEY-WIDE OR HOA FOR MAINTENANCE OF A PRIVATE DRAINAGE SYSTEM

NOTE

LOT 210 IN PHASE I (PA 48) WILL BE USED FOR OPEN SPACE, EMERGENCY ACCESS AND FOR UTILITIES

PHASING PLAN

- ▲ PRIMARY ACCESS
- SECONDARY ACCESS
- E EMERGENCY ACCESS



COUNTY OF RIVERSIDE
TENTATIVE TRACT MAP
NO. 38131
SCHEDULE "A"
EASEMENT NOTES & PHASING MAP

REV	DATE	DESCRIPTION	APPROVED
6/22		REVISED PER COUNTY COMMENTS	
3/22		REVISED PER COUNTY COMMENTS	

KA ENGINEERING
LAND PLANNING
SURVEYING
 Engineering, Inc.
 357 N. SHERIDAN STREET
 SUITE 117
 CORONA, CALIFORNIA 92728
 TEL. (951) 279-1800
 FAX (951) 279-4380

Drawn By: _____ PREPARED: AUGUST 2021 DWG. NO. 3
 Checked By: _____ SHEET
 Scale: AS SHOWN OF 3 SHEETS

MENIFEE NORTH

SPECIFIC PLAN No. 260, AMENDMENT No. 4

APPLICANT:

WSI LAND HOLDINGS, LLC

3161 Michelson Drive, Suite 425

Irvine, CA 92612

(949) 383-4134

Contact: Brian Hardy

PREPARED BY:

T&B PLANNING, INC.

3200 El Camino Real, Suite 100

Irvine, CA 92602

(714) 505-6360

Contact: Joel Morse

IN CONSULTATION WITH:

K&A ENGINEERING – *Civil Engineer*

BRIGHTVIEW DESIGN GROUP – *Landscape Architect*

SECOND SCREENCHECK DRAFT: NOVEMBER 2022

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I. AMENDMENT 4 SUMMARY OF CHANGES

Amendment No. 4 to the MENIFEE NORTH Specific Plan (SP260-A4) increases the Specific Plan area by 5.9 acres, modifies the Land Use Designations in the portion of approved Planning Area 45 east of Leon Road, establishes three new Planning Areas (47, 48 and 49), increases the total number of residential units in the Specific Plan by 543 units (from 2,025 to 2,0798) and increases the Commercial Retail acreage within the Specific Plan by 5.1 acres, from 58.2 to 63.3 acres.

Amendment No. 4 increases the total Specific Plan acreage by 5.9 acres and approved maximum number of residential units by 543 units, while modifying the Specific Plan text and graphics to:

1. **Planning Area 45** - Reduce the acreage and unit count of the portion of Planning Area 45 west of Leon Road to 27.3 acres with 115 dwelling units;
 - a. The portion of Planning Area 45 west of Leon Road was incorrectly mapped in the approved SP260, A2, SC2 document.
 - b. Amendment No. 4 has corrected Planning Area 45's boundary and modified the acreage of Planning Area 45 to match the County of Riverside's GIS/Parcel Database. As a result, the acreage of the portion of Planning Area 45 west of Leon decreased by 11.5 acres from 38.8 acres to 27.3 acres.
 - i. Reduction of the acreage increased the Planning Area 45 density from 3.6 to 4.2 du/ac, and remains designated Medium Density Residential.
 - ii. The unit allocation was recalculated based on the proportional acreages (east and west of Leon). Planning Area 45, east of Leon Road received 56.2% of the acreage (35.0 ac) and the portion west of Leon Road receiving 43.8% of the acreage (27.3 ac), then multiplying the acreage by 4.2 DU/AC. This results in 115 units west of Leon Road.
2. **Planning Areas 47 and 49** – Re-configure the portion of Planning Area 45 east of Leon Road to establish one (1) residential Planning Area 47 and one (1) commercial retail Planning Area 49;
 - a. Planning Area 47, at 29.5 acres consist of 1142 dwelling units at a density of 3.98 du/ac and a mini-park, with a designation of Medium Density Residential (MDR);
 - b. Planning Area 49 at 5.1 acres, would be designated Commercial Retail (CR).
3. **Planning Area 48** – Incorporate and establish a new 17.4-acre area into the Specific Plan boundary as Planning Area 48 located south of Watson Road, between Juniper Flats Road and Leon Road;
 - a. Planning Area 48 is designated Medium High Density Residential (MHDR) and provide 878 dwelling units at a density of 5.04 du/ac.
4. Establish minimum 4,320 s.f. lot sizes within Planning Area 47 and minimum 3,780 s.f. lot sizes within Planning Area 48;
5. Provide ~~the option for~~ a Mini-Park within Planning Area 47;
6. Other, non-substantive changes throughout the document to accommodate these modifications.

Amendment No. 4 also includes a technical correction for the roadway and right-of-way designation of Juniper Flats Road from “Secondary Highway (100’ R.O.W.)” to “Collector Street (74’ R.O.W.)”, to provide consistency of the roadway classifications with the Harvest Valley/Winchester Area Plan. Substantial Conformance No. 2 did not update the designation of Juniper Flats Road to reflect the modified status of Juniper Flats Road to “Collector Street” in the 2015 Harvest alley/Winchester Area Plan Circulation Plan.

Amendment No. 4 re-designates Juniper Flats Road from Secondary Highway (100' R.O.W.) to Collector Street (74' R.O.W.) to resolve this error.

The modifications provided by Amendment No. 4 are summarized in Table SC-1, *Specific Plan No. 260, Amendment No. 4 (Area of Change)*, and, Table SC-2 - *Specific Plan No. 260, Amendment No. 4 (Entire SP Area)*.

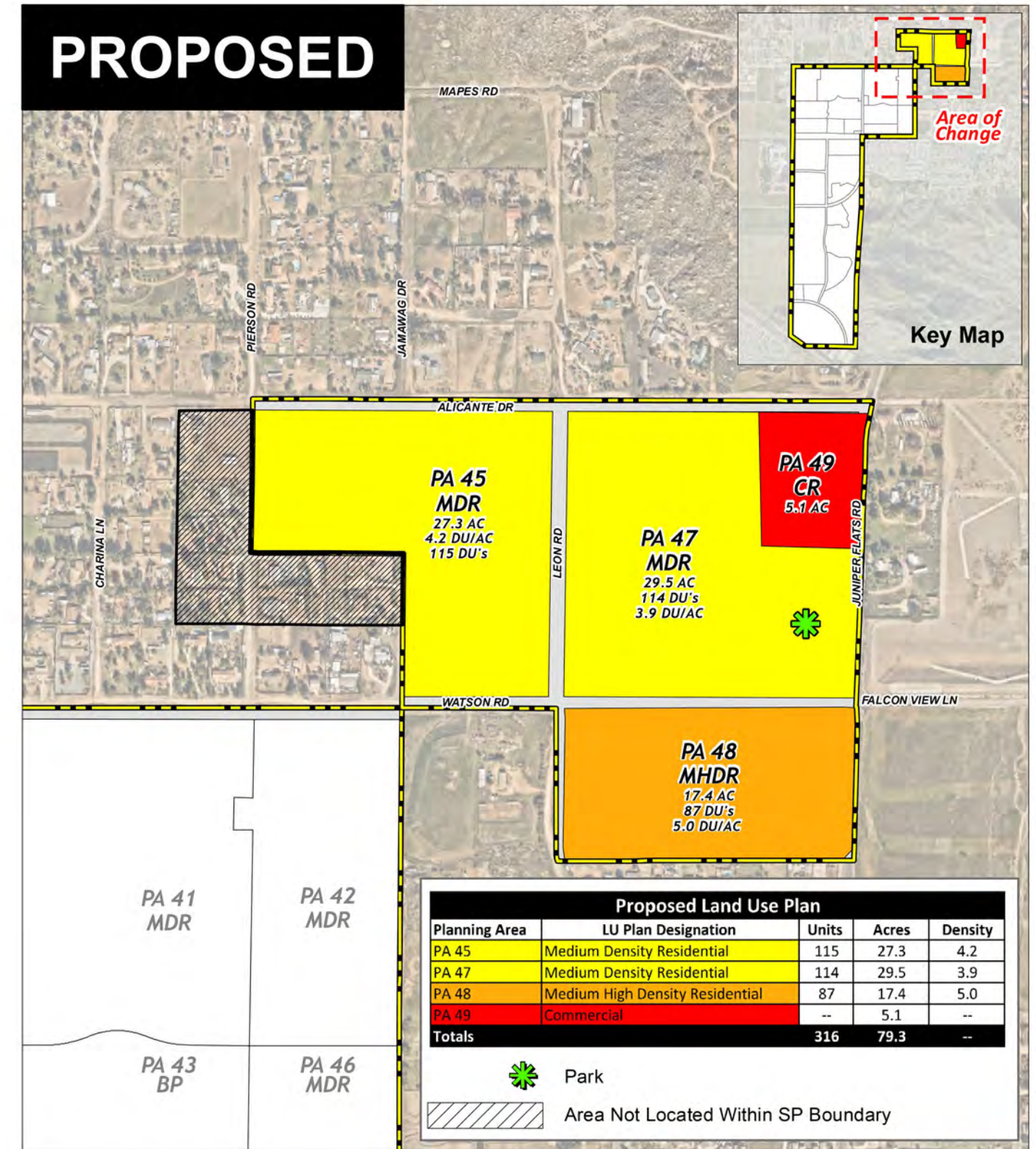
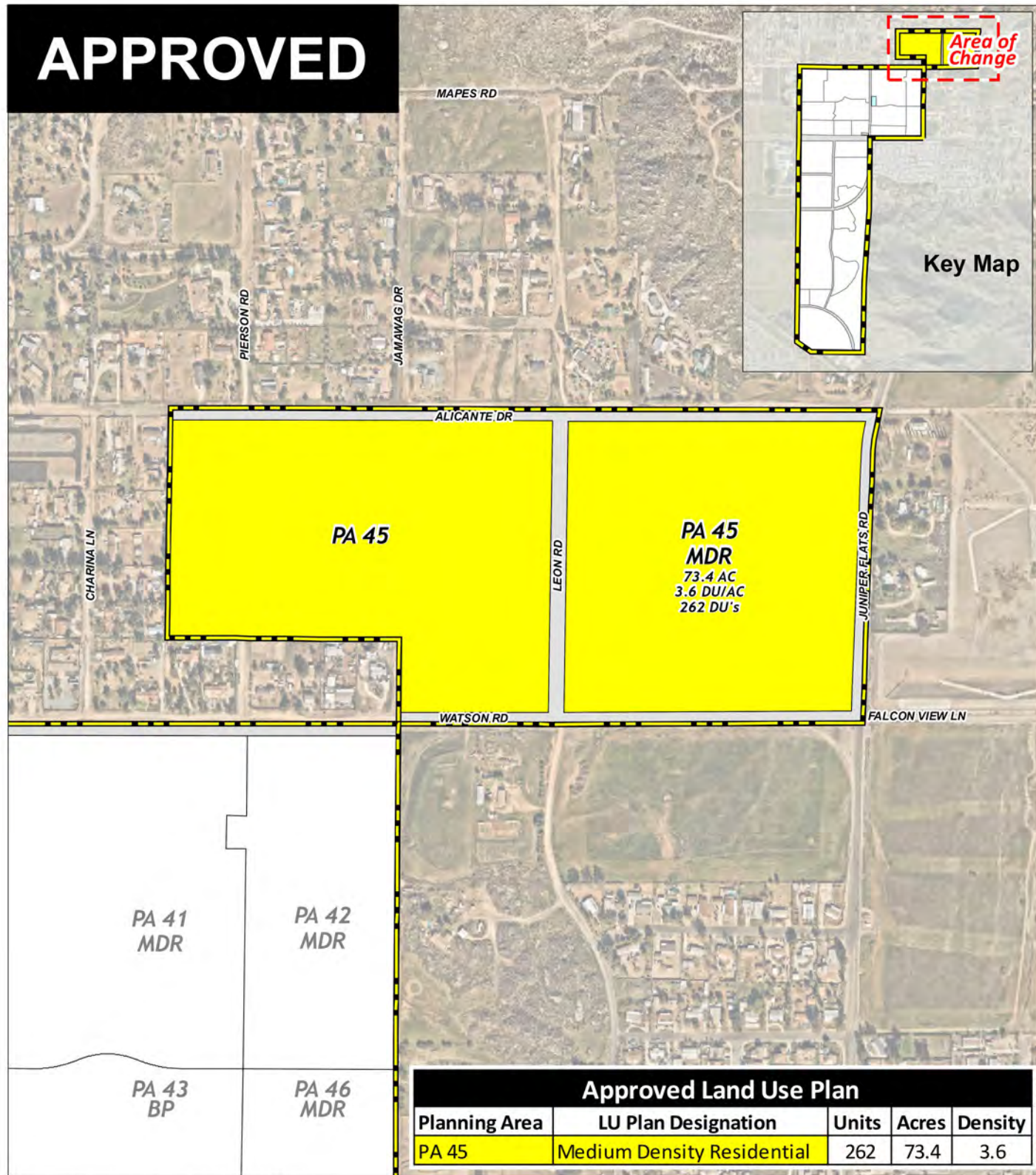
Figure I-1, *Land Use Plan Comparison – Areas of Change*, provides a graphic comparison between the adopted Land Use Plan for MENIFEE NORTH Specific Plan, Amendment No. 2, Substantial Conformance No. 2 and the proposed Land Use Plan for Amendment No. 4.

Table SC-1 - Specific Plan No. 260, Amendment No. 4 (Area of Change)

Menifee North Specific Plan No. 260 Amendment No. 2 Substantial Conformance No. 2					Menifee North Specific Plan No. 260 Amendment No. 4				
PA	Land Use	Target Units	Acres	Target Density	PA	Land Use	Target Units	Acres	Target Density
45	Medium Density Residential	262	73.4	3.6	45	Medium Density Residential	115	27.3	4.2
--	--	--	--	--	47	Medium Density Residential	112	29.5	3.8
--	--	--	--	--	48	Medium-High Density Residential	88	17.4	5.1
--	--	--	--	--	49	Commercial Retail	--	5.1	--
--	Major Roads	--	74.8	--	--	Major Roads	--	74.8	--
Total		262	148.2	--	Total		315	154.1	--

Table SC-2 - Specific Plan No. 260, Amendment No. 4 (Entire SP Area)

Land Use	Target Units	Acres	Target Density	Land Use	Target Units	Acres	Target Density
Medium Density Residential	1,772	438.3	4.1	Medium Density Residential	1,737	421.7	4.0
Medium-High Density Residential	253	44.6	5.7	Medium-High Density Residential	341	62	5.4
Business Park	--	21.8	--	Business Park	--	21.8	--
Commercial Retail	--	58.2	--	Commercial Retail	--	63.3	--
Mixed Use Planning Area	--	18.5	--	Mixed Use Planning Area	--	18.5	--
Light Industrial	--	18.8	--	Light Industrial	--	18.8	--
Public Facility	--	9.9	--	Public Facility	--	9.9	--
Open Space – Recreation	--	5.5	--	Open Space – Recreation	--	5.5	--
Open Space – Conservation	--	102.8	--	Open Space – Conservation	--	102.8	--
Open Space – Water	--	33.8	--	Open Space – Water	--	33.8	--
Utility Easements/Existing Uses	--	0.6	--	Utility Easements/Existing Uses	--	0.6	--
Major Roadways	--	74.8	--	Major Roadways	--	74.8	--
Fire Station	--	1.6	--	Fire Station	--	1.6	--
Total	2,025	829.2	--	Total	2,078	835.1	--



Sources: ESRI, Nearmap Aerial (2021), RCTLMA (2021)

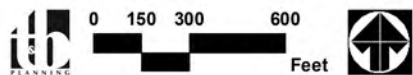


FIGURE I-1

LAND USE PLAN COMPARISON

I. SUMMARY OF CHANGES

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

II. SUMMARY

A. INTRODUCTION

Section 65450 of the California Government Code grants local government agencies the authority to prepare specific plans of development for any area covered by a General Plan, for the purpose of establishing systematic methods of implementation of the agency's General Plan. A Specific Plan, as the name implies, is specific in nature as compared to a General Plan. It is designed to address site specific issues such as building setbacks and visual appearance, as well as project-wide concerns such as circulation and provisions for utilities. A General Plan does not show local streets, individual parcels or specific land uses. While a General Plan examines an entire City or County, a Specific Plan will concentrate on the individual development issues of a particular project or region. The Riverside County General Plan has established objectives which mandate the preparation of individual Specific Plan documents in order to ensure that new developments meet the basic standards of environmental safety, infrastructure, quality of structural design, site planning and contain the provisions to maintain aesthetic quality and cultural identity. Most importantly, the Specific Plan has strong implications on government budgeting decisions.

The *MENIFEE NORTH* Specific Plan provides the County of Riverside, as well as future developers, community groups and community service districts, with a comprehensive set of plans, regulations, conditions and programs for guiding the systematic development of the project. The Specific Plan also implements each applicable Element of the Riverside County General Plan. The *MENIFEE NORTH* Specific Plan assures that development of the proposed land use plan will proceed as a coordinated project involving the orderly and systematic construction of residential, commercial, industrial and open space uses and the provisions of public infrastructure and services necessary to ensure that Riverside County standards for orderly growth are implemented.

The project area primarily consists of vacant land with some existing scattered residential and commercial uses surrounded by either fragmented residential, commercial, agricultural or some limited forms of professional office land uses. When fully developed, the *MENIFEE NORTH* Specific Plan will provide a balance of residential land uses varying in density from 3.0 dwelling units per acre to 5.7 dwelling units per acre. The project will also include one elementary school site, a community park, and natural open space, as well as commercial retail, business park, mixed use planning areas, and light industrial uses.

The residential products to be provided will serve to meet a market need in this urbanizing portion of southwest Riverside County, while maintaining a sensitive approach to development relative to existing streets and anticipated traffic volumes that are planned for within the Riverside County Circulation Plan and Master Plan of Highways.

MENIFEE NORTH will provide a community which offers a unique living and working environment, providing a range of residential densities, commercial and industrial square footage and park and open space amenities consistent with the on-going development in the Southern Perris Valley region. Many of the future residents, who are projected to range from first-time to move-up buyers, will be able to take advantage of amenities offered in the *MENIFEE NORTH* Specific Plan project area.

~~In addition to the *MENIFEE NORTH* Specific Plan, the accompanying Environmental Impact Report (EIR), was prepared under the authority of the County of Riverside and is in compliance with the California Environmental Quality Act (CEQA) and County of Riverside Guidelines. The EIR is intended to serve as the project-wide Master Environmental Document for the *MENIFEE NORTH* Specific Plan and all subsequent development projects undertaken pursuant to and within the Specific Plan boundaries. Furthermore, the EIR is an informational document designed to provide decision-makers and members of the general public with a full understanding of the potential environmental effects of the development proposal. Together, the Specific Plan and EIR provide a path to properly develop the site, taking into account all local goals, objectives and environmental considerations.~~

B. PROJECT SUMMARY SPECIFIC PLAN HISTORY

Since the adoption of Amendment No. 2 to the *MENIFEE NORTH* Specific Plan in 2008, the City of Menifee incorporated, resulting in an approximately 775.4-acre portion of the adopted Specific Plan west of Briggs Road being removed from County of Riverside jurisdiction. In January 2016, Substantial Conformance No. 1 to Amendment No. 2 was adopted and modified the Specific Plan boundary, removing all areas west of Briggs Road from the *MENIFEE NORTH* Specific Plan. The revised limits of the *MENIFEE NORTH* Specific Plan were reduced to approximately 829.2 acres east of Briggs Road, west of Juniper Flats Road, south of Alicante Drive, and north of Matthews Road and provides for a wide range of land uses including 2,025 residential units.

Amendment No. 4 to the *MENIFEE NORTH* Specific Plan (SP260-A4) increases the Specific Plan area by 5.9 acres, modifies the Land Use Designations in the portion of approved Planning Area 45 east of Leon Road, establishes three new Planning Areas (47, 48 and 49), increases the total number of residential units in the Specific Plan by 543 units (from 2,025 to 2,079~~8~~) and increases the Commercial Retail acreage within the Specific Plan by 5.1 acres, from 58.2 to 63.3 acres.

Amendment No. 4 also includes a technical correction for the roadway and right-of-way designation of Juniper Flats Road from “Secondary Highway (100’ R.O.W.)” to “Collector Street (74’ R.O.W.)”, to provide consistency of the roadway classifications with the Harvest Valley/Winchester Area Plan. Substantial Conformance No. 2 did not update the designation of Juniper Flats Road to reflect the modified status of Juniper Flats Road to “Collector Street” in the 2015 Harvest alley/Winchester Area Plan Circulation Plan. Amendment No. 4 re-designates Juniper Flats Road from Secondary Highway (100’ R.O.W.) to Collector Street (74’ R.O.W.) to resolve this error.

In addition to the *MENIFEE NORTH* Specific Plan, the accompanying Environmental Impact Report (EIR), was prepared under the authority of the County of Riverside and is in compliance with the California Environmental Quality Act (CEQA) and County of Riverside Guidelines. The EIR is intended to serve as the project-wide Master Environmental Document for the *MENIFEE NORTH* Specific Plan and all subsequent development projects undertaken pursuant to and within the Specific Plan boundaries. Furthermore, the EIR is an informational document designed to provide decision-makers and members of the general public with a full understanding of the potential environmental effects of the development

proposal. Together, the Specific Plan and EIR provide a path to properly develop the site, taking into account all local goals, objectives and environmental considerations.

Each subsequent Amendment and Substantial Conformance to the Specific Plan also required compliance with CEQA and County of Riverside Guidelines, which also provided information to decision-makers of the general public with an understanding of potential environmental effects of any changes to the original Specific Plan. Similar to the original Specific Plan and EIR, each of the subsequent documents continue to provide a path to properly develop the site. In 2007, in conjunction with Specific Plan Amendment No. 1, a Mitigation Negative Declaration (MND) was prepared in compliance with CEQA. An MND was also prepared in 2008 in conjunction with Specific Plan Amendment No. 2. In 2014, in conjunction with implementing Tentative Tract Map (TTM) No. 36430, a MND was prepared. In 2016, in conjunction with Substantial Conformance No. 1, an Addendum to the Menifee North EIR was prepared in compliance with CEQA. An Addendum to the EIR was also prepared in 2020 in conjunction with Substantial Conformance No. 2. In 2022, an Addendum to the Menifee North EIR and Addendum to the 2014 TTM No 36430 MND was prepared with the implementation of Specific Plan Amendment No. 4.

~~The *MENIFEE NORTH* Specific Plan provides the County of Riverside, as well as future developers, community groups and community service districts, with a comprehensive set of plans, regulations, conditions and programs for guiding the systematic development of the project. The Specific Plan also implements each applicable Element of the Riverside County General Plan. The *MENIFEE NORTH* Specific Plan assures that development of the proposed land use plan will proceed as a coordinated project involving the orderly and systematic construction of residential, commercial, industrial and open space uses and the provisions of public infrastructure and services necessary to ensure that Riverside County standards for orderly growth are implemented.~~

~~The project area primarily consists of vacant land with some existing scattered residential and commercial uses surrounded by either fragmented residential, commercial, agricultural or some limited forms of professional office land uses. When fully developed, the *MENIFEE NORTH* Specific Plan will provide a balance of residential land uses varying in density from 3.0 dwelling units per acre to 5.7 dwelling units per acre. The project will also include one elementary school site, a community park, and natural open space, as well as commercial retail, business park, mixed use planning areas, and light industrial uses.~~

~~The residential products to be provided will serve to meet a market need in this urbanizing portion of southwest Riverside County, while maintaining a sensitive approach to development relative to existing streets and anticipated traffic volumes that are planned for within the Riverside County Circulation Plan and Master Plan of Highways.~~

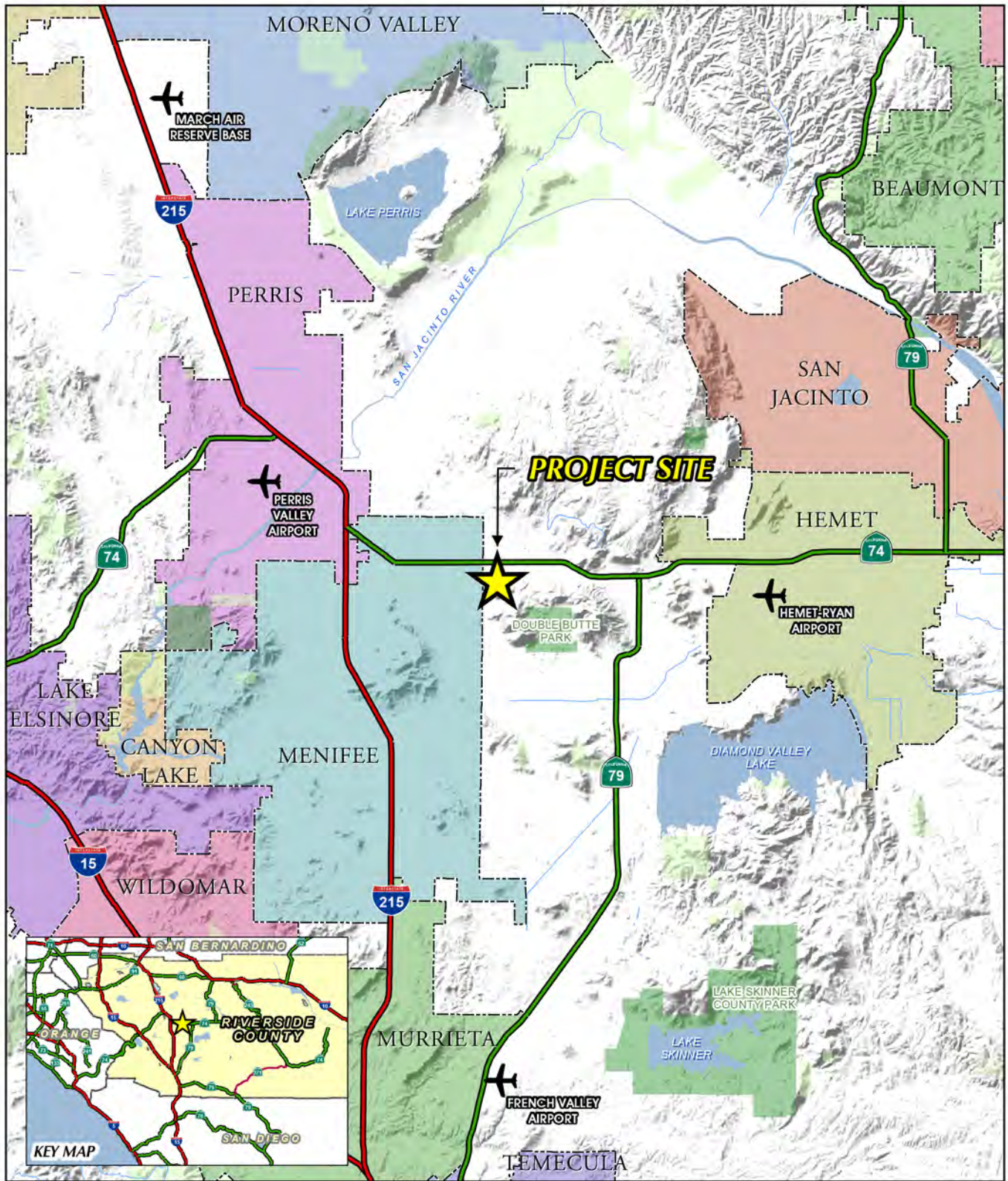
~~*MENIFEE NORTH* will provide a community which offers a unique living and working environment, providing a range of residential densities, commercial and industrial square footage and park and open space amenities consistent with the on-going development in the Southern Perris Valley region. Many of the future residents, who are projected to range from first-time to move-up buyers, will be able to take advantage of amenities offered in the *MENIFEE NORTH* Specific Plan project area.~~

The *MENIFEE NORTH* Specific Plan can be briefly summarized as follows:

Table I Land Use Summary

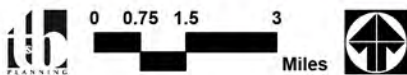
LAND USE	ACREAGE	DWELLING UNITS / ACRE	MAXIMUM DWELLING UNITS	PERCENTAGE
RESIDENTIAL				
7,200 SF Medium	104.3	3.79	391	12.5%
6,000 SF Medium	135.8	4.239	564	16.3%
5,000 SF Medium ^{(1),(2)}	152.1	4.4	670	18.2%
4,320 SF Medium	29.5	3.98	1142	3.5%
3,780 SF Medium High	17.4	5.04	878	2.1%
3,500 SF Medium High	44.6	5.7	253	5.3%
RESIDENTIAL SUBTOTAL	483.7	4.35	2,0798	57.9%
NON-RESIDENTIAL				
Commercial Retail	63.3	-	-	7.6%
Light Industrial	18.8	-	-	2.3%
Mixed Use Planning Area	18.5	-	-	2.2%
Business Park	21.8	-	-	2.6%
Public Facility	9.9	-	-	1.2%
Open Space-Recreation ⁽³⁾	5.5	-	-	0.7%
Open Space-Conservation	102.8	-	-	12.3%
Open Space-Water	33.8	-	-	4.0%
Public Facility Fire Station	1.6	-	-	0.2%
Major Roads	74.8	-	-	9.0%
Easements/Existing Uses	0.6	-	-	0.1%
NON-RESIDENTIAL SUBTOTAL	351.4	-	-	42.1%
PROJECT TOTAL	835.1	4.35	2,0798	100.0%

- (1) The maximum density may be increased to 6.0 du/ac with a 5,000 square foot lot size in Planning Area 34 in either of the following two circumstances:
 - (a) The project is designed for and restricted to senior citizen housing; or
 - (b) The project is a mobile home park or mobile home subdivision.
 If this option is elected, the maximum number of dwelling units shown in Table II for Planning Areas 34 may increase, however, the overall maximum number of dwelling units for the entire Specific Plan may not exceed 2,025.
- (2) The overall number of units for Planning Areas 33B and 34 is restricted to require that several neighborhoods of differing lot sizes be developed.
- (3) Mini-parks are required in Planning Areas 26, 32, 35, 37, and 47. A 4.9-acre Public Park is located within Planning Area 25. A Mini-Park may be provided in Planning Area 47, final location to be determined with implementing project(s).



Source(s): ESRI, RCTLMA (2019)

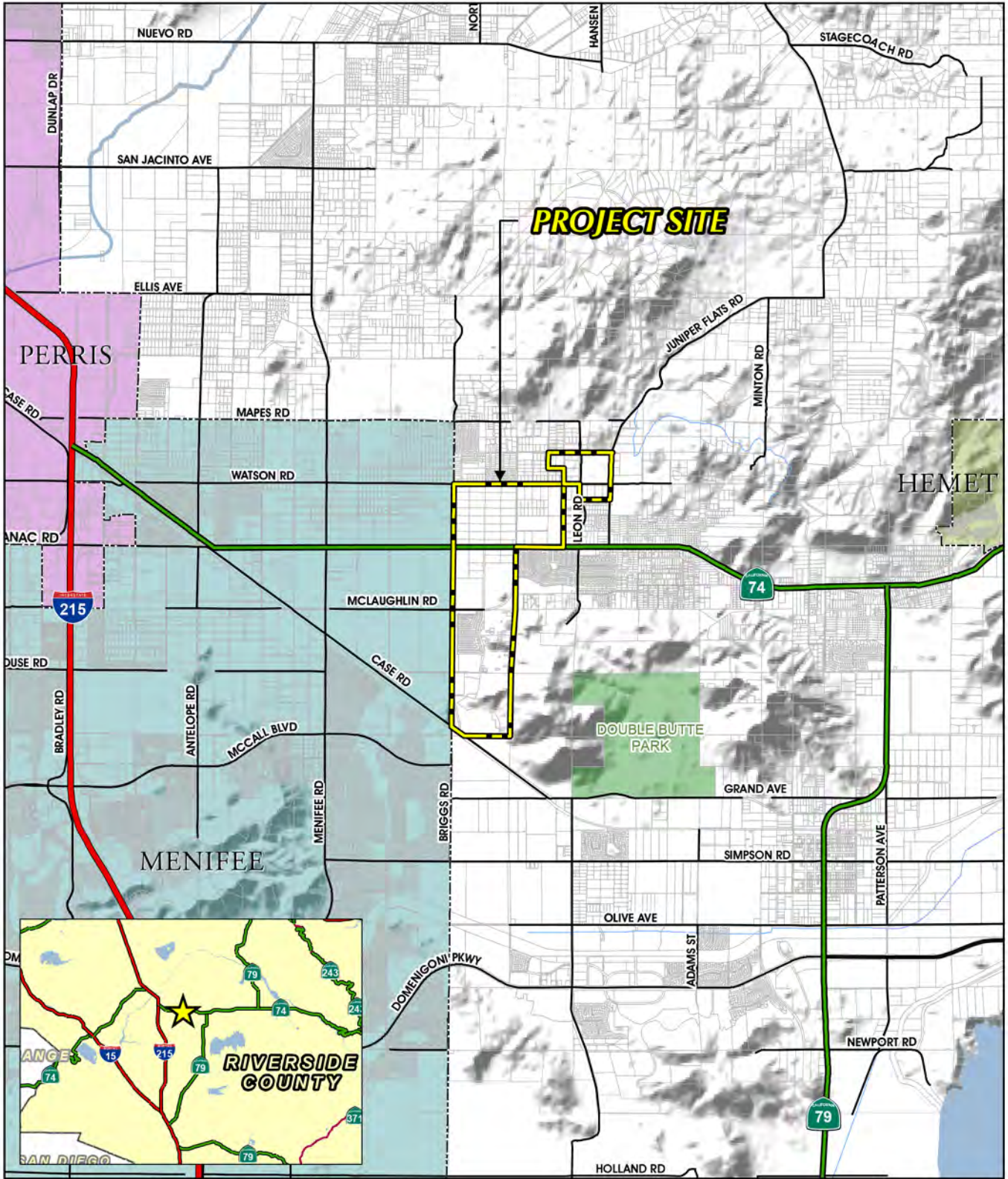
FIGURE II-1



REGIONAL MAP

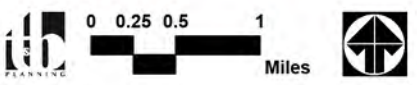
II. SUMMARY

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4



Source(s): Esri, Neamap Imagery (2021), RCTLMA (2021)

FIGURE II-2



VICINITY MAP
II. SUMMARY

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

III. SPECIFIC PLAN

A. DEVELOPMENT PLANS AND STANDARDS

1. PLANNING OBJECTIVES

This Specific Plan has been prepared within the framework of a detailed and comprehensive multi-disciplinary planning program. Issues such as engineering feasibility, market acceptance, economic viability, County Comprehensive General Plan goals and objectives, development phasing and local community goals have been fully examined and considered. To further ensure the environmental compatibility, aesthetic satisfaction and functional integrity of the plan, specific planning goals and objectives were identified. With this analysis and the specific project goals and objectives in mind, the *MENIFEE NORTH* Specific Plan:

- Provides a development plan of superior environmental sensitivity including a high quality of visual aesthetics, suppression of noise, protection of health and safety, and the promotion of the community and region.
- Considers topographic, geologic, hydrologic and environmental opportunities and constraints to create a design that generally conforms to the character of the land by retaining and utilizing basic existing landforms as much as possible.
- Reflects anticipated market needs and public demand by providing a range of housing types which will be marketable within the developing economic profile of Southern Perris Valley Area as well as the County of Riverside.
- Provides residential uses with specific emphasis on employing natural and created open space for a heightened aesthetic environment.
- Provides direct and convenient access to clustered neighborhoods via a convenient and efficient circulation system.
- Provides additional employment opportunities for the current and future residents of the region and surrounding communities.
- Creates a unique residential character that provides for a distinct environment through architectural treatment, viewshed, and natural terrain.

2. SPECIFIC LAND USE PLAN

a. Project Description

When completed, the *MENIFEE NORTH* project area will be composed of a high-quality mix of residential uses varying in density from 3.0 du/ac to 5.7 du/ac. These residential uses will be constructed in compliance with a site design that is consistent with the Riverside County General Plan. The residential products to be provided will meet a market need in the urbanizing Southern Perris Valley community. When fully developed, a maximum of 2,079~~8~~ homes will be constructed in *MENIFEE NORTH*. The units will be developed across a

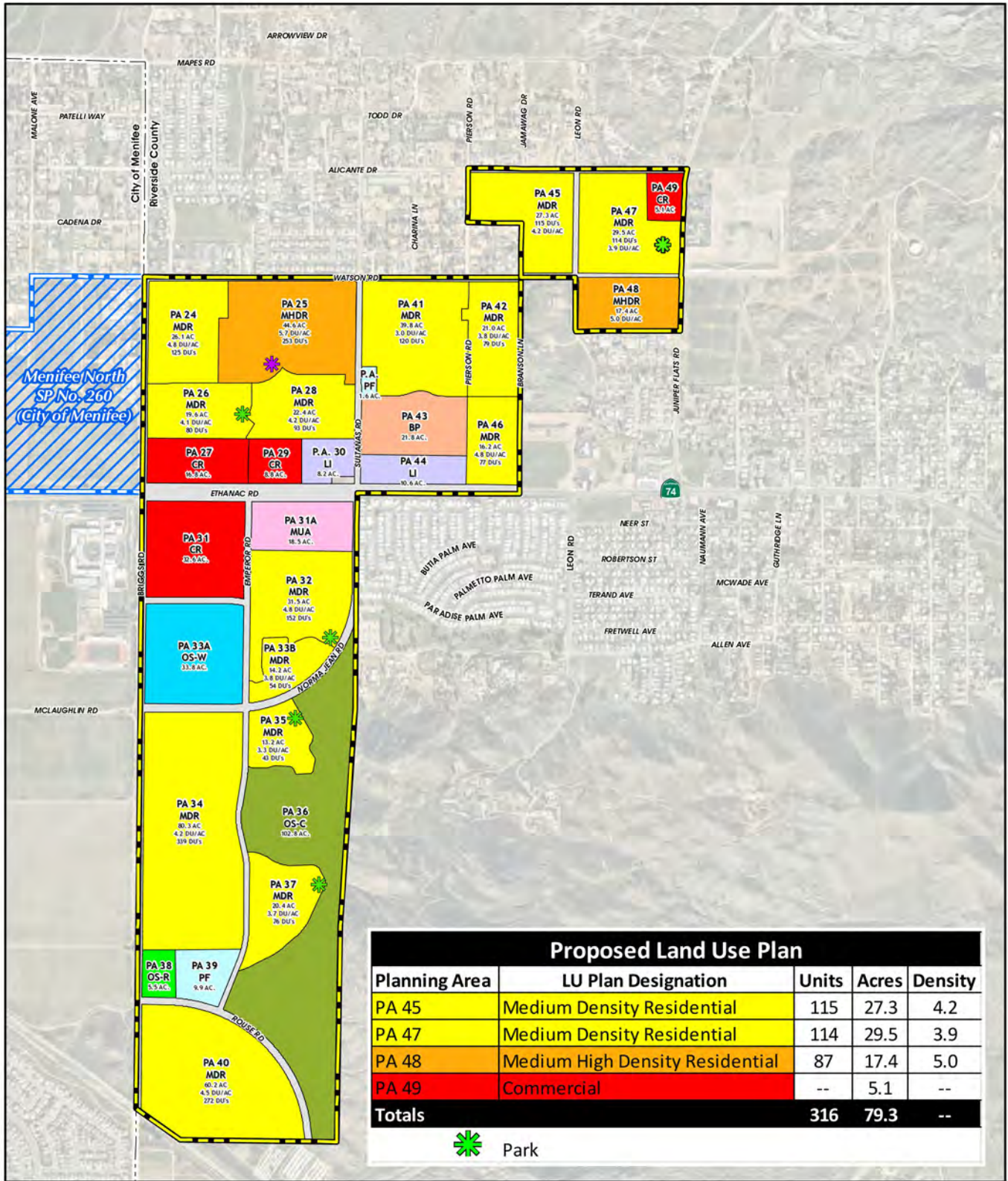
spectrum of lot sizes as depicted in the *Specific Plan Land Use Plan* (Figure III-1). In order to create a full service, balanced community, the project plans for one elementary school site, one community park, natural open space, commercial retail, business park, light industrial, and mixed use planning areas.

The project site has been divided into 29 planning areas on the basis of logical, separate units of development. Specific information on each of the planning areas within *MENIFEE NORTH* is provided within Section III.B; Planning Area Development Standards, in the Detailed Land Use Summary (Table II) and in Figures III-14 through 26).

The proposed land uses within the *MENIFEE NORTH* Specific Plan include:

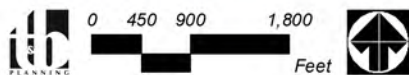
- RESIDENTIAL - The Specific Plan was formulated based on the goal of providing a variety of housing types, styles, sizes and values to attract a variety of homebuyers. The residential element of the plan accounts for 483.7 acres of the project site, containing 2,079~~8~~ dwelling units. Residential densities within the project will net 4.3~~2~~ du/ac and gross 2.5~~4~~ du/ac overall. The housing mix will be spread over four density ranges varying from Medium to High, as described below.
 - Medium Density Residential (7,200 s.f. lots, 3.7~~6~~ du/ac) - consists of 391 dwelling units on 104.3 acres in Planning Areas 41, 42, 45, and 46. Lot sizes in these areas will be a minimum of 7,200 square feet.
 - Medium Density Residential (6,000 s.f. lots, 4.2 du/ac) - consists of 564 dwelling units on 135.8 acres in Planning Areas 26, 28, 35, 37, and 40. Lot sizes in these areas will be a minimum of 6,000 square feet.
 - Medium Density Residential (5,000 s.f. lots, 4.4 du/ac) - consists of 670 dwelling units on 152.1 acres in Planning Areas 24, 32, 33B and 34. Lot sizes in these areas will be a minimum of 5,000 square feet.
 - Medium Density Residential (4,320 s.f. lots, 3.9~~4~~~~2~~ du/ac) - consists of 114~~2~~ dwelling units on 29.5 acres in Planning Area 47. Lot sizes in these areas will be a minimum of ~~6,000~~4,320 square feet.
 - Medium High Density Residential (3,500 s.f. lots, 5.7 du/ac) - consists of 253 dwelling units on 44.6 acres in Planning Areas 25. Lot sizes in these areas will be a minimum of 3,500 square feet.
 - Medium High Density Residential (3,780 s.f. lots, 5.0~~4~~ du/ac) - consists of 87~~8~~ dwelling units on 17.4 acres in Planning Areas 48. Lot sizes in these areas will be a minimum of 3,780 square feet.

The maximum density of Planning Area 34 may be increased to 6.0 du/ac with a 5,000 square foot lot minimum if the planning area is designed for and restricted to senior citizen housing or if the planning area is a mobile home park or mobile home subdivision. Additional residential opportunities are also provided by Planning Area 31A which could accommodate high density residential uses within a mixed use/neighborhood commercial environment.



Source(s): ESRI, Near Map Aerial (2021), RCTLMA (2021)

FIGURE III-1



SPECIFIC PLAN LAND USE PLAN

III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

- COMMERCIAL RETAIL - *MENIFEE NORTH* will contain several commercial sites totaling 63.3 acres, proposed for Planning Areas 27, 29, 31, and 49. The commercial uses proposed will serve residents of *MENIFEE NORTH*, off-site users and persons employed in the area. The commercial sites are located to provide easy access for both on-site residents and the regional population. The provision of commercial uses is important not only for convenience, but also to satisfy commercial needs of the future population in this region.
- LIGHT INDUSTRIAL - The Light Industrial uses, totaling 18.8 acres of land, are proposed for Planning Areas 30, and 44. This land use is intended to provide areas for wholesale commercial, business/office, supportive commercial, and light industrial uses.
- BUSINESS PARK - The Business Park uses, totaling 21.8 acres of land, are proposed for Planning Area 43. This land use is intended to provide development areas for professional office and related supportive commercial uses as well as some limited types of "clean" light industrial uses.
- MIXED USE PLANNING AREA - The Mixed Use area in Planning Area 31A encompasses 18.5 acres and provides a mix of limited commercial and high density residential land uses.
- PUBLIC FACILITY – One elementary school site is planned within *MENIFEE NORTH*, containing a total of 9.9 acres, located in Planning Area 39. The Specific Plan satisfies the standards of the Perris Union and Romoland School Districts for the reservation of school sites as the project is implemented over the phasing period.
- OPEN SPACE-RECREATION - The *MENIFEE NORTH* project offers various sites for active recreation, highlighted by a 4.9-acre public park in Planning Area 25 and a 5.5-acre community park in Planning Area 38, strategically located adjacent to the elementary school site to provide excellent opportunities for sharing facilities. In addition, mini-parks are required in portions of Planning Areas 26, 32, 35, and 37. Including 5.3 acres of mini-parks, the project provides 15.7 acres of neighborhood parkland. A Mini-Park ~~may is~~ be provided in Planning Area 47, to be determined with implementing project(s).
- OPEN SPACE-CONSERVATION - Approximately 102.8 acres of the Specific Plan area will remain as natural open space, preserved in Planning Area 36. This open space area includes the preservation of a significant hillside in the eastern portion of the site.
- ROADS - Major roadways totaling 74.8 acres will be developed in conjunction with the proposed project. The Riverside County Master Plan of Streets and Highways will adequately serve future traffic volumes for the region. On-site traffic will be handled by a hierarchical roadway system consisting of Expressway, Urban Arterial Highway, Major Highway, Secondary Highway, Industrial Collector, Collector and local roadways ranging in right-of-way width from 56 feet to 220 feet.

b. Land Use Development Standards

In order to ensure the orderly and sensitive development of the land proposed for *MENIFEE NORTH*, special techniques or mitigations have been created for each planning area. These area-specific standards, discussed in detail in Section III.B., Planning Area Development Standards, will assist in accommodating the proposed development and provide adequate transitions among neighboring land uses.

In addition to these specific techniques, project-wide development standards also have been prepared which complement the unique conditions within each planning area. These general standards are:

- 1) The total Specific Plan area shall be developed with a maximum of 2,078 dwelling units on 835.1 acres, as illustrated on the *Specific Plan Land Use Plan* (Figure III-1). General uses permitted will include a variety of residential, including mixed use planning areas, commercial retail, business park, light industrial, public facilities, open space-recreation, open space-conservation, and circulation as delineated on the *Specific Plan Land Use Plan*. These permitted uses are also depicted in Figures III-14- through 26, which support the discussion of individual planning areas.
- 2) Uses and development standards will be in accordance with the County of Riverside Zoning Code and will be defined by Specific Plan objectives, future detailed plot plans and potential conditional use permits as appropriate.
- 3) Standards relating to signage, landscaping, parking and other related design elements will conform to the County of Riverside Zoning Ordinance No. 348. When appropriate and necessary to meet the goals of this Specific Plan, the standards contained within this document will exceed the zoning code requirements. In addition, a Specific Plan Zoning Ordinance will be processed concurrently with this Specific Plan.
- 4) All project lighting shall be in accordance with applicable Riverside County standards, including Ordinance No. 655 regarding Mt. Palomar Observatory standards.
- 5) Development of the property shall be in accordance with the mandatory requirements of all Riverside County ordinances including Ordinances No. 348 and 460. Development shall conform substantially with adopted Specific Plan No. 260 as filed in the office of the Riverside County Planning Department, unless otherwise amended. This Specific Plan conforms with state laws.
- 6) Except for the Specific Plan Zoning Ordinance adopted concurrently with this Specific Plan, no portion of this Specific Plan which purports or proposes to change, waive or modify any ordinance or other legal requirement for the development shall be considered to be part of the adopted Specific Plan.
- 7) A land division filed for the purpose of phasing or financing shall not be considered an implementing development application provided that if the maintenance organization is a property owner's association, the legal documentation necessary to establish the association shall be recorded concurrently with the recordation of the final map.
- 8) Common areas identified in the specific plan shall be owned and maintained as follows:
 - a) A permanent master maintenance organization shall be established for the Specific Plan area, to assume ownership and maintenance responsibility for all common recreation, open space, and landscaped areas (including those in commercial retail, light industrial, business park, and mixed use planning areas). The organization may be public or private. Merger with an area-wide or regional organization shall satisfy this condition provided that such organization is legally and financially capable of assuming the responsibilities for ownership and maintenance. If the organization is a private association, then neighborhood associations shall be established

for each residential development, where required, and such associations may assume ownership and maintenance responsibility for neighborhood common areas.

- b) Unless otherwise provided for in these standards, common areas shall be conveyed to the maintenance organization as implementing development is approved or any subdivision is recorded.
 - c) The maintenance organization shall be established prior to or concurrent with the recordation of the first land division or issuance of any building permit for any approved development permit (use permit, plot plan, etc.). The ownership and maintenance responsibility shall be identified for each open space lot at the time Tentative Subdivision Maps are filed.
- 9) The permittee shall defend, indemnify, and hold harmless the County of Riverside or its agents, officers, and employees from any claim, action, or proceeding against the County of Riverside or its agents, officers, or employees to attack, set aside, void, or annul an approval of the County of Riverside, its advisory agencies, appeal boards or legislative body concerning Specific Plan No. 260. The County of Riverside will promptly notify the permittee of any such claim, action, or proceeding against the County of Riverside and will cooperate fully in the defense. If the County fails to promptly notify the permittee of any such claim, action, or proceeding or fails to cooperate fully in the defense, the permittee shall not, thereafter, be responsible to defend, indemnify, or hold harmless the County of Riverside.
 - 10) Prior to issuance of building permits for construction of any use contemplated by this approval, the applicant shall first obtain clearance from the County of Riverside Planning Department verifying that all pertinent conditions of Specific Plan approval have been satisfied for the phase of development in question.
 - 11) An environmental assessment shall be conducted for each Tract, Plot Plan, Specific Plan Amendment or any other discretionary permit required to implement the Specific Plan. At a minimum, the environmental assessment shall utilize the evaluation of impacts addressed in the EIR No. 329 prepared for this Specific Plan.
 - 12) Lots created pursuant to this Specific Plan and any subsequent tentative maps shall be in conformance with the development standards of the specific plan zone herein applied to the property.
 - 13) Development applications which incorporate common areas shall be accompanied by design plans for the common areas, specifying location and extent of landscaping, irrigation systems, structures and circulation (vehicular, pedestrian and/or bicycle.)
 - 14) Passive solar heating techniques will be employed whenever possible within the project. Passive solar systems do not utilize sophisticated hardware. Passive systems involve orienting buildings properly, planting trees to take advantage of the sun, seeing that roof overhangs are adequate, making sure that walls are properly insulated and installing simple heat storage systems.
 - 15) If necessary, roadways, infrastructure and open space will be coordinated by and paid for through an assessment or community services district to facilitate construction, maintenance and management.

- 16) Final development densities for each planning area shall be determined through the appropriate development application up to the maximum density identified, based upon but not limited to the following:
- a) Adequate availability of services.
 - b) Adequate access and circulation.
 - c) Sensitivity to landforms.
 - d) Innovation in housing types and design.
 - e) Density transfer.
 - f) Sensitivity to neighborhood design through lot and street layouts.
 - g) Lot sizes as proposed in this Specific Plan.
 - h) Density bonuses for affordable or senior citizen housing.
- 17) Areas designated as open space that will be conveyed within parcel boundaries to individual property purchases shall be deed restricted so as to create open space easements and prohibit grading, construction or other development activity in such open space.
- 18) Prior to the issuance of building permits, improvement plans for developed common open space areas, including irrigation plans, shall be submitted for Planning Department approval for the stage of development in question. Irrigation plans shall be certified by a landscape architect.
- 19) For the security and safety of future residents, the applicant and/or developer shall incorporate the following design concepts within each individual tract:
- a) Circulation for pedestrians, vehicles and police patrols.
 - b) Lighting of streets, walkways and bikeways.
 - c) Visibility of doors and windows from the street and between buildings, where practical.
 - d) Fencing heights and materials.
- 20) It is anticipated that maintenance associations, CSAs or CSDs, if formed, will be established as follows:
- The master property owner's association shall be charged with the unqualified right to assess their own individual owners who own individual units for reasonable maintenance and management costs which shall be established and continuously maintained. The property owner's association shall be responsible for private roads, parking, open space area, signing, landscaping, irrigation, common areas and other responsibilities as necessary.
- 21) Prior to the issuance of final grading plans for individual planning areas, a detailed noise analysis shall be prepared to determine the height and location of noise barriers needed to achieve acceptable noise levels.
- 22) Designation and/or dedication of parkland and open space acreage, necessary to satisfy both County and State requirements, will be based on the final number of dwelling units and subsequent population generated by the *MENIFEE NORTH* Specific Plan as adopted by the Riverside County Board of Supervisors, unless otherwise amended.

Table II
MENIFEE NORTH
DETAILED LAND USE SUMMARY

DESIGNATION	PLANNING AREA	ACREAGE	TARGET DENSITY	DWELLING UNITS
RESIDENTIAL				
Medium – 7,200 SF min	41	39.8	3.0	120
	42	21.0	3.8	79
	45	27.3	4.2	115
	46	16.2	4.8	77
SUBTOTAL - MEDIUM - 7,200 SF MIN.		104.3	3.79	391
Medium – 6,000 SF min	26	19.6	4.1	80
	28	22.4	4.2	93
	35	13.2	3.3	43
	37	20.4	3.7	76
	40	60.2	4.5	272
SUBTOTAL - MEDIUM - 6,000 SF MIN.		135.8	4.23-9	564
Medium - 5,000 SF min.	24	26.1	4.8	125
	32	31.5	4.8	152
	33B	14.2	3.8	54
	34 ⁽¹⁾	80.3	4.2	339
SUBTOTAL - MEDIUM - 5,000 SF MIN.		152.1	4.4	670
Medium – 4,320 SF min.	47	29.5	3.98	1142
SUBTOTAL - MEDIUM – 4,320 SF MIN.		29.5	3.98	1142
Medium High – 3,780 SF min.	48	17.4	5.04	878
SUBTOTAL – MEDIUM HIGH – 3,780 SF MIN.		17.4	5.04	878
Medium High – 3,500 SF min.	25	44.6	5.7	253
SUBTOTAL - MEDIUM HIGH - 3,500 SF MIN.		44.6	5.7	253
RESIDENTIAL SUBTOTALS		483.7	4.35	2,0798
NON-RESIDENTIAL				
	43	21.8	—	—
SUBTOTAL - BUSINESS PARK		21.8	—	—
	27	16.8	—	—
	29	8.8	—	—
	31	32.6	—	—
	49	5.1	--	--
SUBTOTAL - COMMERCIAL RETAIL		63.3	—	—
Mixed Use Planning Area	31A	18.5	—	—
SUBTOTAL – MIXED USE PLANNING AREA		18.5	—	—
	30	8.2		

Table II
MENIFEE NORTH
DETAILED LAND USE SUMMARY

DESIGNATION	PLANNING AREA	ACREAGE	TARGET DENSITY	DWELLING UNITS
	44	10.6		
SUBTOTAL - LIGHT INDUSTRIAL		18.8	—	—
	39	9.9		
SUBTOTAL - PUBLIC FACILITY		9.9	—	—
	38	5.5		
SUBTOTAL - OPEN SPACE-RECREATION		5.5	—	—
Open Space-Conservation	36	102.8	—	—
Open Space-Water	--	33.8	—	—
Utility Easements/Existing Uses	--	0.6	—	—
Major Roadways	--	74.8	—	—
Fire Station	--	1.6	—	—
NON-RESIDENTIAL SUBTOTALS		351.4	—	—
PROJECT TOTALS		835.1	4.35	2,0798

¹ The maximum density may be increased to 6.0 du/ac with a 5,000 square foot lot size in Planning Area 34 in either of the following two circumstances:

- a. The project is designed for and restricted to senior citizen housing, or
- b. The project is a mobile home park or mobile home subdivision

If this option is elected, the maximum number of dwelling units shown in Table II may be exceeded up to a new maximum which is listed in the Planning Area description for the relevant Planning Area. The overall maximum number of dwelling units for the entire Specific Plan may not exceed 2,025,815.

² Mini-parks are required in Planning Areas 26, 32, 35, 37, and 47. A 4.9-acre Public Park is located within Planning Area 25. A Mini-Park ~~may be~~ provided in Planning Area 47, to be determined with implementing project(s).

3. CIRCULATION PLAN

a. Circulation Plan Description

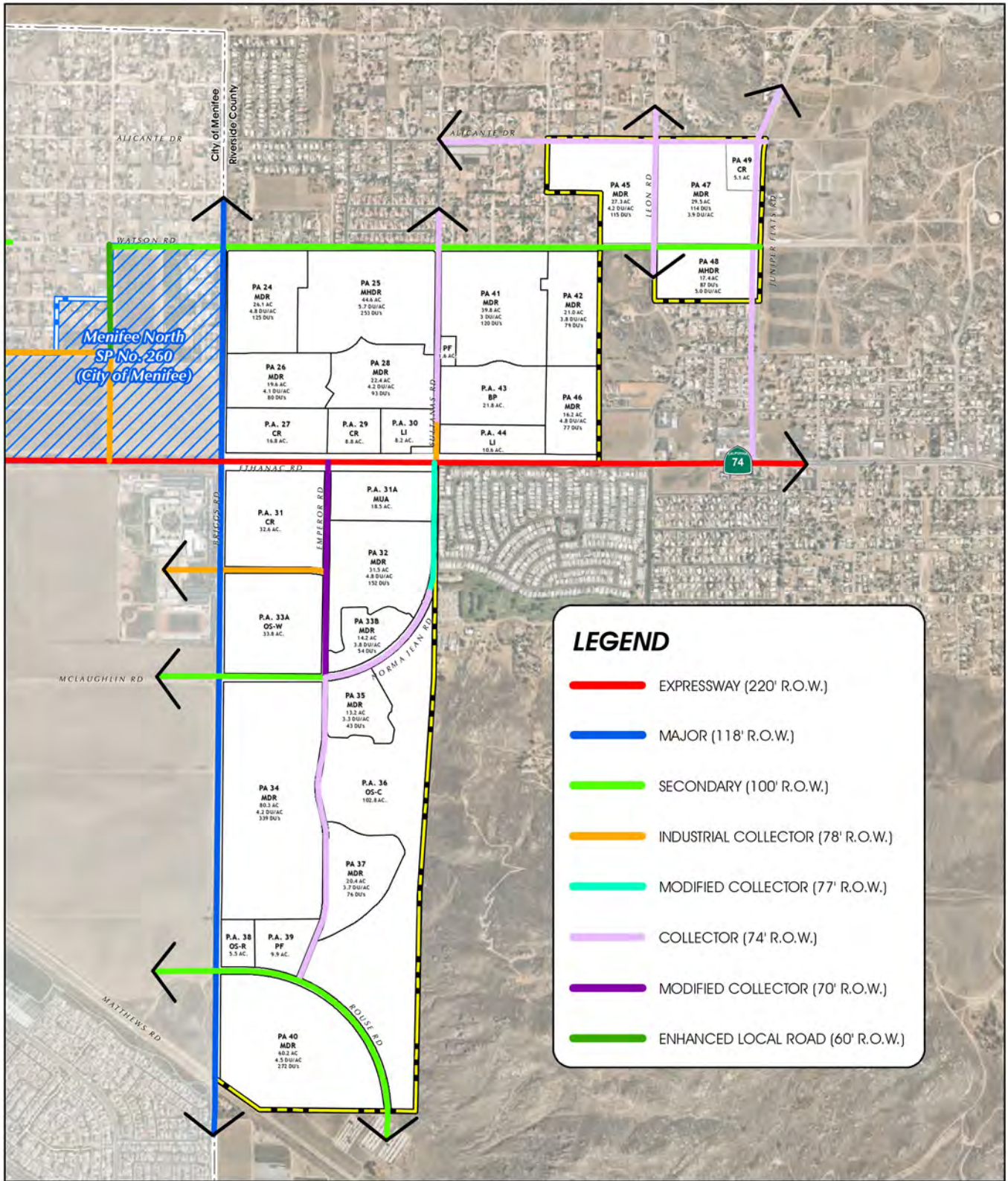
As the result of a thorough traffic analysis conducted by Robert Kahn, John Kain and Associates, Inc. (See Technical Appendices), a project roadway concept has been developed for *MENIFEE NORTH*, as illustrated in the *Specific Plan Circulation Plan* (Figure III-2). In addition to the project roadway concept, Amendment No. 2 has modified the circulation plan to be in conformance with the Circulation Element of the Riverside County Integrated Project.

Principal east-west roadway access to and through the site will be provided by the existing Highway 74 and the proposed improved Ethanac Road Corridor. Primary north-south traffic through the project site will be conveyed via Briggs Road, proposed on-site as a 118' ROW Major Highway. On-site circulation will be accommodated by a network of public and private roadways. Each major roadway's location and size are designed, as illustrated on Figure III-2, to facilitate the efficient movement of traffic throughout the site.

The main objective of the Circulation Plan is to provide direct and convenient access to individual neighborhoods and supportive land uses through a safe and efficient system of urban arterial, arterial, secondary, industrial collector, collector, and local roadways. Non-vehicular circulation will be promoted through the provision of bicycle lanes, sidewalks and a community trail.

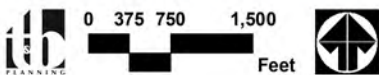
b. Circulation Development Standards

- 1) The proposed Circulation Plan provides an efficient traffic design that meets the needs of the project. The on-site system depicted on the *Specific Plan Circulation Plan* (Figure III-2) has been derived from the Master Circulation Plan outlined in the project Traffic Analysis and will serve as the composite Circulation Plan for the *MENIFEE NORTH* Specific Plan. (See Technical Appendices.) The illustrated, on-site roadway improvements will be phased in accordance with this plan.
- 2) Heavy through-traffic should be eliminated from residential neighborhoods. Major roadways should be implemented as non-access roadways, with residential neighborhoods served by smaller residential collectors.
- 3) Each subdivision shall comply with the on-site and off-site street improvement recommendations and mitigation measures outlined in subsequent traffic studies for each individual project.



Source(s): ESRI, Near Map Aerial (2021), RCTLMA (2021)

FIGURE III-2



SPECIFIC PLAN CIRCULATION PLAN

III. SPECIFIC PLAN
 MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

4) Roadway cross-sections are depicted in Figure III-3, Figure III-4, and Figure III-4A. The Roadway Cross-Section provided in Figure III-4A apply to the roadways within and/or fronting Planning Areas 45, 47, 48, and 49. On-site roads will be constructed as:

- Expressway (220' ROW)
- Major Highway (118' ROW)
- Secondary Highway (100' ROW)
- Industrial Collector (78' ROW)
- Modified Collector (77' ROW)
- Collector (74' ROW)
- Modified Collector (70' ROW)
- Enhanced Local Road (60' ROW)
- Local streets (56' ROW)

4a) The minimum interior local street width may be reduced from the 60-foot standard to no less than 56 feet for the purpose of providing more landscaped area. Any approved reduction shall be accomplished by reducing the width of the asphalt travel lanes by two feet in each direction. Any reduction in street width shall be used solely to augment landscape areas on the exterior street and shall be done on a 1:1 basis.

4b) A reduction in the width of a local street may be appropriate to reduce traffic speeds on internal residential streets. Such reduction in pavement width must be utilized to accomplish other design objectives, e.g., enhancing the buffer treatment of reverse frontage lots, providing for sheltered parking opportunities, or incorporating bus stop turnouts.

4c) Parking on both sides of the street requires a minimum curb-to-curb width of 40 feet.

5) Landscape requirements shall be in accordance with the Roadway Landscape Treatments as depicted in Section IV, Design Guidelines.

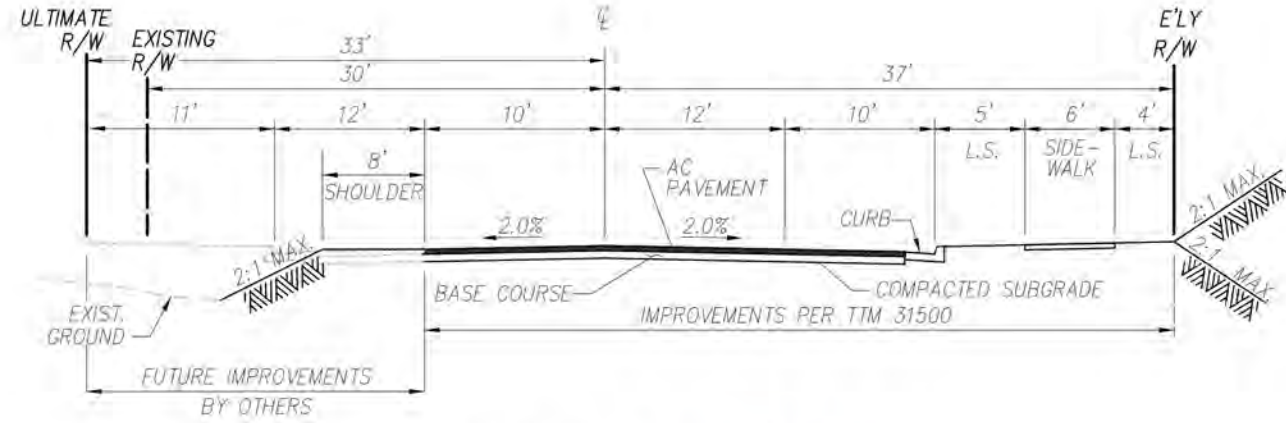
6) Major roadway improvements shall be financed through an assessment district or similar financing mechanism.

7) The project proponent shall participate in the Traffic Signal Mitigation Program as approved by ~~the Board of Supervisors~~Ordinance No. 659.

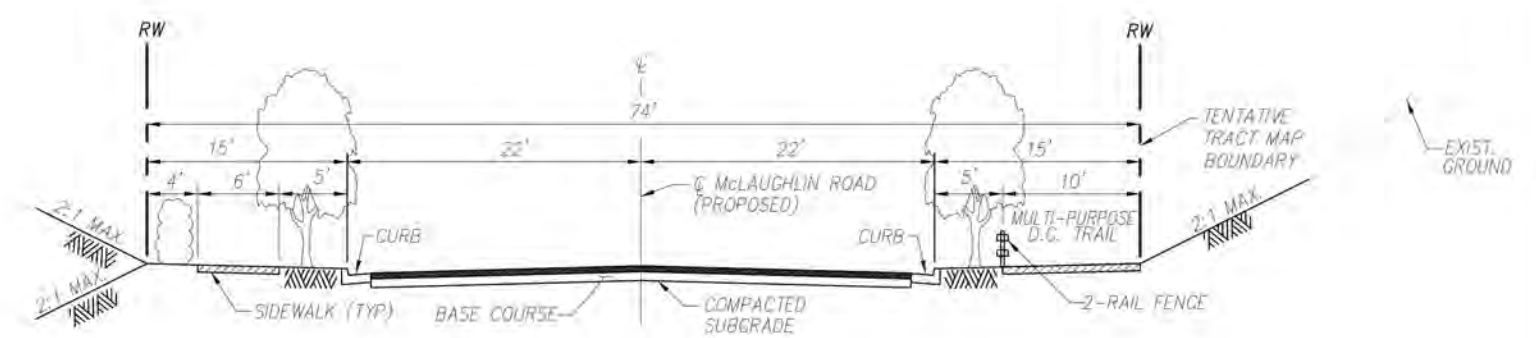
8) All projects, including subdivisions and plot plans within this Specific Plan, shall be subject to the Development Monitoring Program as described in the Development Monitoring Program (Section II.C) of this Specific Plan.

9) Any application for any subdivision within the Specific Plan boundary (including a Schedule I Parcel Map) shall cause the design and construction of the Specific Plan master planned infrastructure within the final map boundaries, with the exception of a division of land that has no parcel less than 40 acres or that is not less than a quarter of a quarter section.

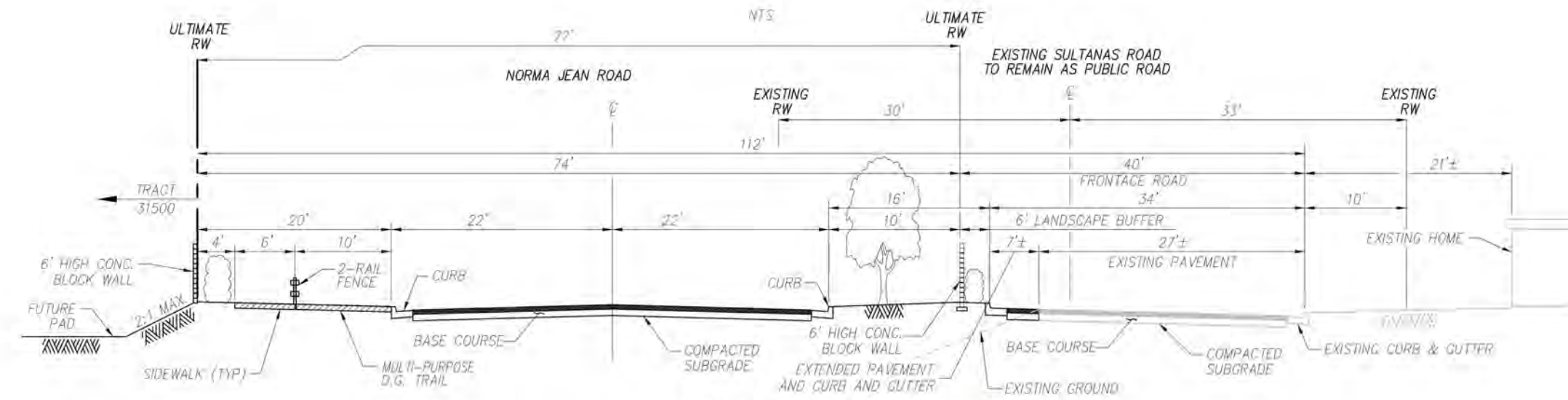
- 10) All roadways intersecting four lane facilities or greater shall be a minimum of 66 feet of right-of-way and constructed in accordance with Standard 103, Ordinance 461 from the four lane facility to the nearest intersection.



**MODIFIED COLLECTOR
EMPEROR ROAD - (SEGMENT 1)**
BETWEEN MCLAUGHLIN AND ETHANAC ROAD/SR 74
N.T.S.



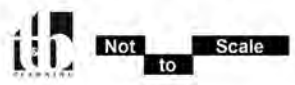
**COLLECTOR
NORMA JEAN ROAD - (SEGMENT 2A)**
BETWEEN EMPEROR ROAD AND SULTANAS ROAD
N.T.S.



**MODIFIED COLLECTOR
NORMA JEAN ROAD/SULTANAS ROAD - (SEGMENT 2B)**
FROM HIGHWAY 74 TO NORTHERN HALF OF PLANNING AREA 32
N.T.S.

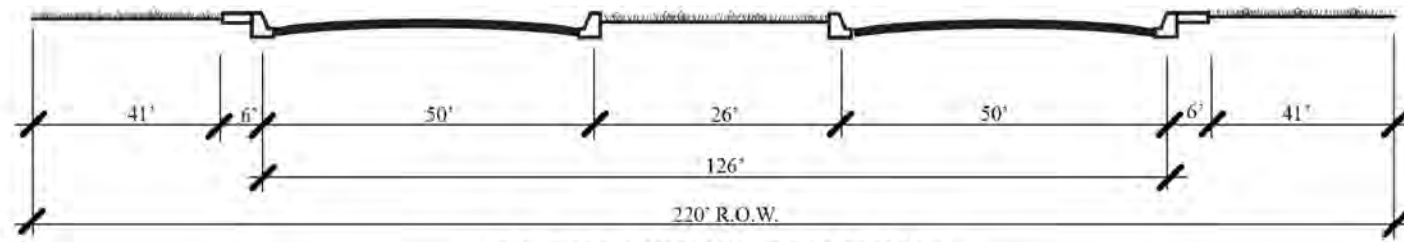
Source(s): ACD Engineering

FIGURE III-3

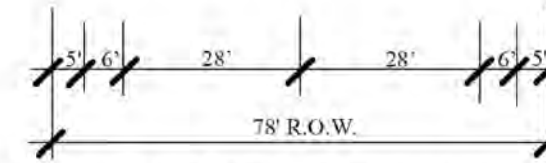


ROADWAY CROSS-SECTIONS (1 OF 3)

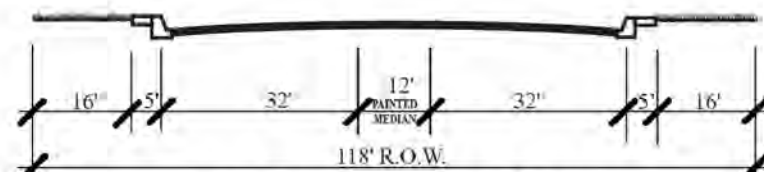
III. SPECIFIC PLAN
MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4



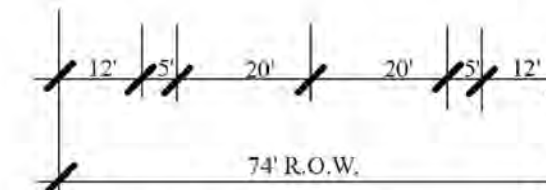
EXPRESSWAY (220' R.O.W.)
Highway 74/Ethanac Road (Briggs to Juniper Flats Road)



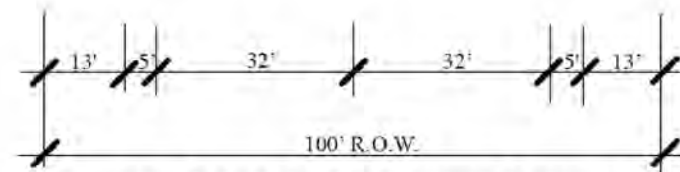
INDUSTRIAL COLLECTOR (78' R.O.W.)
Sultanas Road (Highway 74 to North Boundary of Planning Area 44)
Street "B" (Emperor Road to Briggs Road)



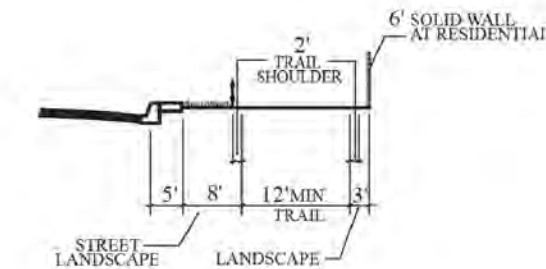
MAJOR (118' R.O.W.)
Briggs Road



COLLECTOR (74' R.O.W.)
Leon Road
Emperor Road (McLaughlin Road to Rouse Road)
Sultanas Road (North Boundary of Planning Area 44 to Watson Road)
Juniper Flats Road (Highway 74 to Alicante Road)



SECONDARY (100' R.O.W.)
Rouse Road
Watson Road



TRAIL OPTION

Source(s): ACD Engineering, RCIP

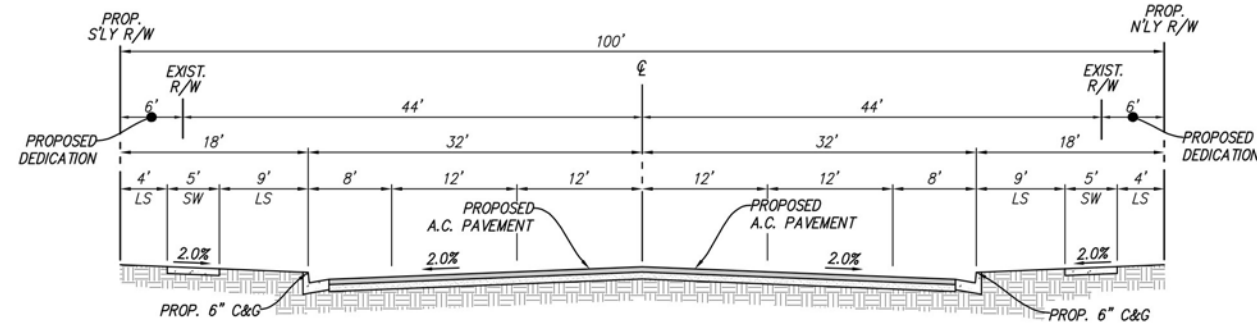
FIGURE III-4

Not to Scale

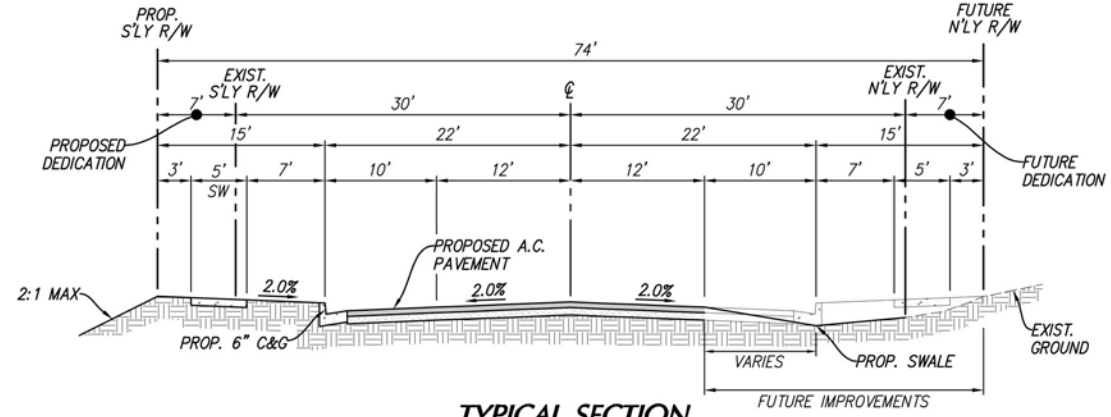
ROADWAY CROSS-SECTIONS (2 OF 3)

III. SPECIFIC PLAN
MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

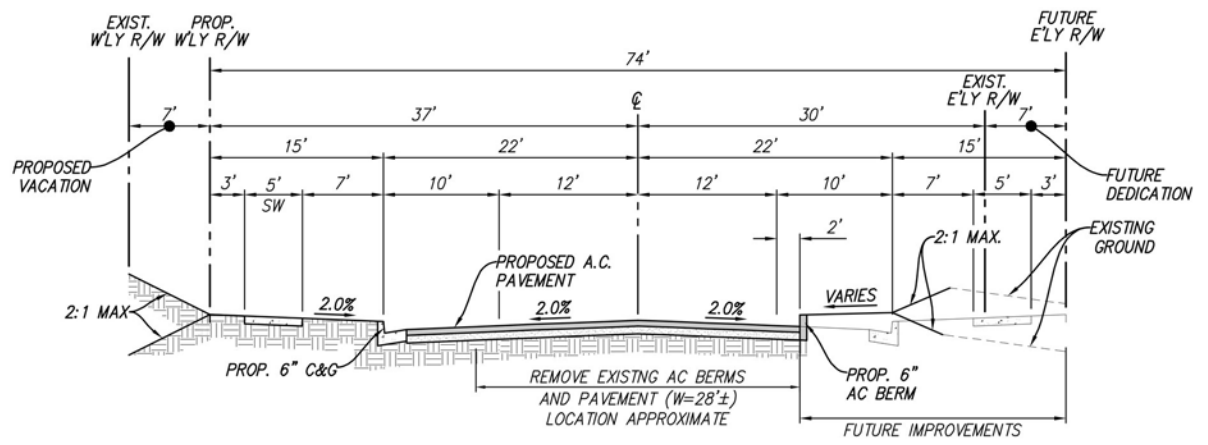
X-X



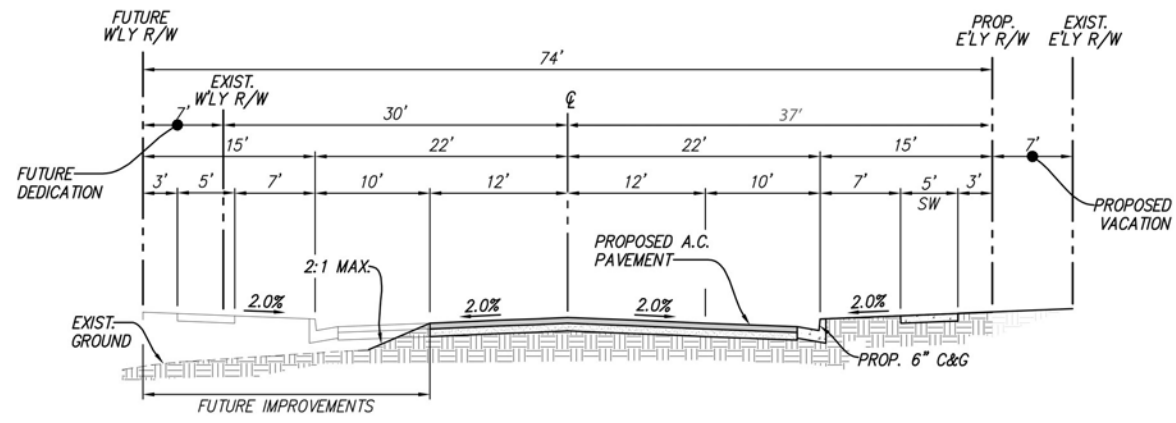
TYPICAL SECTION
 NTS
 SECONDARY HIGHWAY - STD. NO. 94
WATSON ROAD



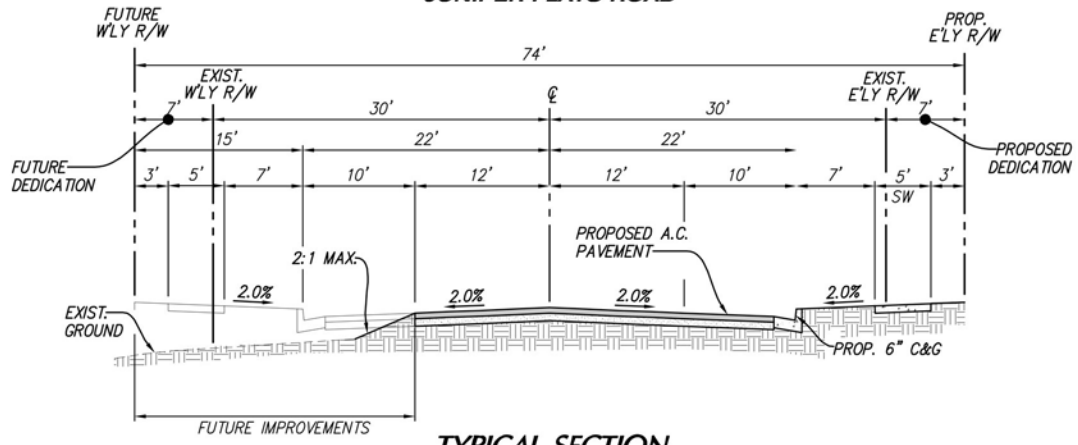
TYPICAL SECTION
 COLLECTOR STREET - STD. NO. 103, SECTION A
ALICANTE DRIVE



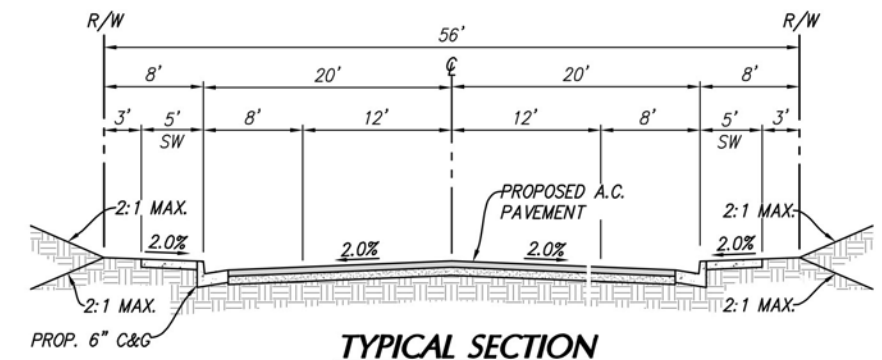
TYPICAL SECTION
 COLLECTOR STREET - STD. NO. 103, SECTION A
JUNIPER FLATS ROAD



TYPICAL SECTION
 COLLECTOR STREET - STD. NO. 103, SECTION A
LEON ROAD - SOUTH OF WATSON ROAD



TYPICAL SECTION
 COLLECTOR STREET - STD. NO. 103, SECTION A
LEON ROAD - NORTH OF WATSON ROAD

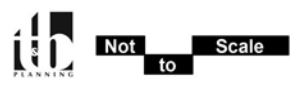


TYPICAL SECTION
 NTS
 INTERIOR LOCAL STREETS - MODIFIED STD. NO. 105, SECTION A
"B" STREET THROUGH "F" STREET, "H" STREET THROUGH "M" STREET AND "N" STREET NORTH OF "K" STREET

Note: The roadway cross-sections in this exhibit apply to Planning Areas 45, 47, 48, & 49, as part of SP260-A4.

Source(s): K & A Engineering (August 2021)

FIGURE III-4A



ROADWAY CROSS-SECTIONS (3 OF 3)

III. SPECIFIC PLAN
 MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

- 11) All typical sections shall be per Ordinance 461, or as approved by the Transportation Department.
- 12) All intersection spacing and/or access openings shall be per Standard 114, Ordinance 461, or as approved by the Transportation Department.
- 13) No textured pavement accents will be allowed within any County right-of-way.
- 14) Mid-block crosswalks are not approved.
- 15) No driveways or access points as shown in this Specific Plan are approved. All access points shall conform to Transportation Department standard access spacing, depending upon the street's classification.
- 16) ~~This Specific Plan proposes drainage facilities in the Watson Road right of way that will be Flood Control District facilities. All other facilities are to be maintained by the Transportation Department~~The existing master planned storm drain facilities within the Watson Road right of way will be maintained by the Riverside County Flood Control and Water Conservation District. All other storm drain facilities, and their maintenance, will be determined through the review of the individual implementing projects.
- 17) Per the Riverside County General Plan, *"Neighborhood Commercial Uses must be located along Secondary or greater highways, at or near intersections with Secondary Highways."*
- 18) The Transportation Department's policy regarding streets adjacent to school sites and park sites requires a minimum of 66' right-of-way (Ordinance No. 461, Standard 1043).
- 19) Any landscaping within public road rights-of-way will require approval by the Transportation Department and assurance of continuing maintenance through the establishment of a landscape maintenance district or similar mechanism as approved by the Transportation Department.
- 20) All bike trails developed as part of this Specific Plan should be designated as Class I bikeways generally located within separate rights-of-way in accordance with the standards contained within Chapter 1000 of the California Department of Transportation - Highway Design Manual (Fourth Edition).
- 21) Right-of-way dedication for Sultanas Road improvements beyond those now existing will be granted from the *MENIFEE NORTH* project site.
- 22) Where the project site boundary abuts Paradise Palm Drive, access to the adjacent golf course will be re-established at no cost to Highland Palms residents as right-of-way improvements in this area proceed.

**Table IIA
CIRCULATION ELEMENT ROADWAYS**

ROADWAY NAME	CURRENT CIRCULATION ELEMENT CLASSIFICATION	PROPOSED CIRCULATION ELEMENT CLASSIFICATION
Highway 74/Ethanac Road	Major 100' ROW	Expressway 220' ROW
Briggs Road	Major 118' ROW	Major 118' ROW
Rouse Road	Secondary 100' ROW	Secondary 100' ROW
Watson Road (Briggs Road to Juniper Flats Road)	Secondary 100' ROW	Secondary 100' ROW
McLaughlin Road (Briggs Road to Emperor Road)	Secondary 100' ROW	Secondary 100' ROW
Juniper Flats Road (Highway 74 to Alicante Drive)	Secondary 100' ROW Collector 74' ROW	Collector 74' ROW
Norma Jean Road (Previously referred to as McLaughlin Road/Sultanas Road) (Highway 74 to the northern half of Planning Area 32)	Secondary 100' ROW	Modified Collector 77' ROW
Norma Jean Road (Previously referred to as McLaughlin Road/Sultanas Road) (Emperor Road to the southern half of Planning Area 32)	Secondary 100' ROW	Modified Collector 74' ROW
Emperor Road (Highway 74 to McLaughlin Road)	Industrial Collector 78' ROW	Modified Collector 70' ROW
Street "B"	Industrial Collector 78' ROW	Industrial Collector 78' ROW
Sultanas Road (Highway 74 to Northern boundary of Planning Area 44)	Industrial Collector 78' ROW	Industrial Collector 78' ROW
Sultanas Road (Watson Road to Northern boundary of Planning Area 44)	Collector 74' ROW	Collector 74' ROW
Emperor Road (McLaughlin Road to Rouse Road)	Collector 74' ROW	Collector 74' ROW
Leon Road	Collector 74' ROW	Collector 74' ROW

4. DRAINAGE PLAN

a. Drainage Plan Description

The *MENIFEE NORTH* Specific Plan is mainly located within the boundaries of the Riverside County Flood Control and Water Conservation District's (RCFCD) Homeland/Romoland Area Drainage Plan (see Figure III-5, *Regional Drainage Plan*). A southern portion of the project site is outside of this area drainage plan, and there are no area drainage plans developed at this time to address drainage issues in this southern area.

The master drainage plan developed by the applicant suggests a combination of master drainage lines proposed by the RCFCD Area Drainage Plan, and new or revised lines per the *MENIFEE NORTH* Specific Plan to be utilized to address off-site drainage issues relative to the site. Figure III-6 outlines the proposed storm drain system within the project site.

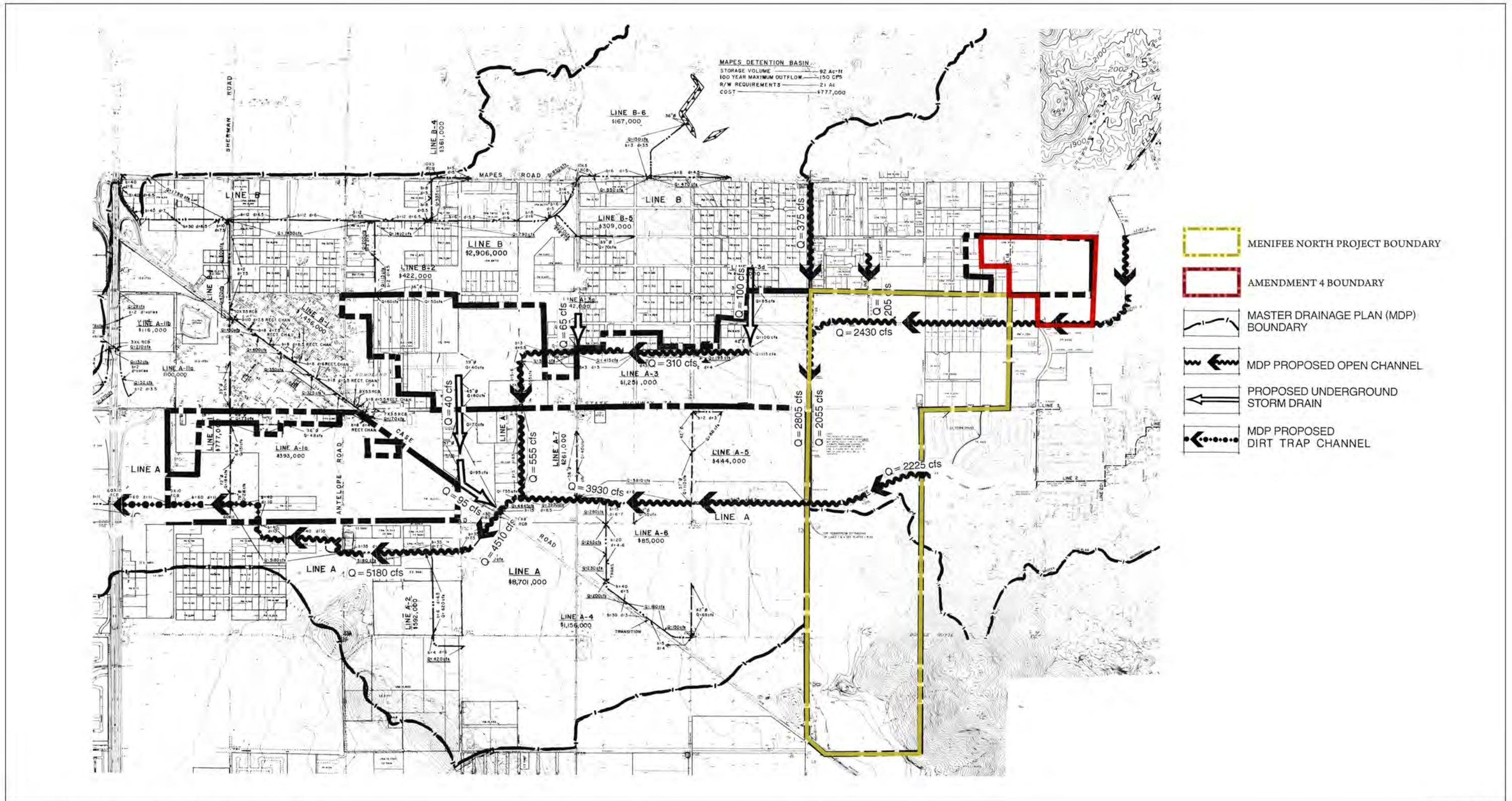
Off-site flows will be intercepted at existing drainage courses where possible, and if necessary, drainage swales will be constructed to concentrate all off-site drainage at proposed inlets on the north project boundary.

The Romoland/Homeland Area Drainage Plan anticipates the construction of storm drain facilities north of *MENIFEE NORTH* to reduce some of the run-off tributary to the north boundary of the project. Since these off-site facilities are not constructed yet, *MENIFEE NORTH* development is responsible to intercept the run-off at its existing conditions. Due to increased run-off in Lines A-3 and A-1, on-site retention basins are proposed in order to reduce flows to designed run-off per the Romoland/Homeland Drainage Plan. Lines 1 and 4 will be constructed per the Homeland/Romoland Area Drainage Plan. A portion of Line A within the Specific Plan area has already been designed and will be utilized in the Specific Plan. On-site regional drainage facilities could be required if storm water exceeds street capacities. The actual size and location of the on-site storm drain system will be determined during design stage of on-site improvement plans.

The construction of Line 1 will cause diversion of flows. This line discharges to proposed Line A per the Master Drainage Plan. The construction of Line A through the site creates a diversion. A portion of Line A from Antelope Road to I-215 has already been designed by RCFCD but it cannot be constructed until the San Jacinto River project is completed. For the portion of Line A through the project to be approved, all downstream facilities including the Antelope Road stormdrain should be established or under construction. A financing mechanism such as a Community Facilities District (CFD) or Assessment District may be formed by all the affected property owners in order to construct the facilities between Antelope Road and the project boundary.

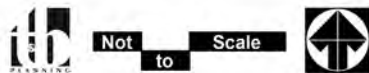
b. Drainage Plan Development Standards

- 1) Drainage and flood control facilities and improvements shall be provided in accordance with Riverside County Flood Control and Water Conservation District requirements.
- 2) It is anticipated that major backbone drainage facilities will be maintained by the County Flood Control District. Local drainage devices within street rights-of-way will be maintained by County Transportation Department.



Source(s): J.F. Davidson & Associates, Riverside County Flood Control and Water District

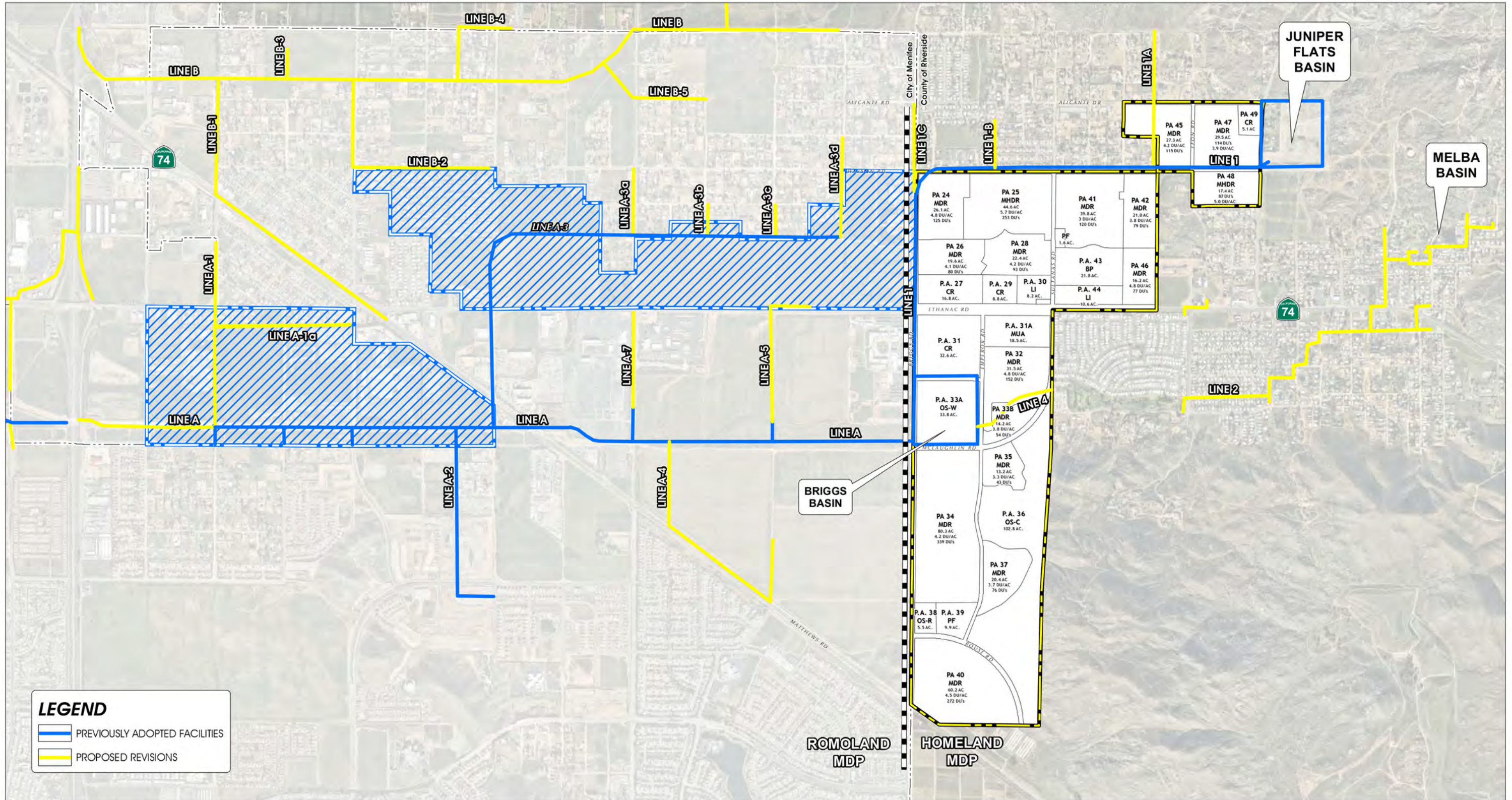
FIGURE III-5



REGIONAL DRAINAGE PLAN

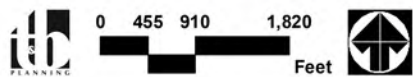
III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4



Source(s): ESRI, Near Map Aerial (2021), RCTLMA (2021)

FIGURE III-6



MASTER DRAINAGE PLAN

III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

- 3) An erosion, sedimentation and flood control plan should be prepared for all development areas draining into the San Jacinto River and Salt Creek. This plan should include no improvements, and should protect these water courses from erosion and siltation during and after construction. Drainage of urban areas should be controlled prior to reaching either the San Jacinto or Salt Creek courseways through the use of velocity reduction devices at the points where run-off enters the waterways.

- 4) All projects proposing construction activities including: cleaning, grading or excavation that results in the disturbance of at least five acres of total land area, or activity which is part of a larger common plan of development of one acre or greater, shall obtain the appropriate NPDES construction permit and pay the appropriate fees. There is a statewide General Permit applicable to most areas of the state; however, because *MENIFEE NORTH* is located in the San Jacinto Watershed, the project is subject to a special NPDES permit (No. CAG 618005) per Order No. 01-34 as adopted by the California Regional Water Quality Control Board, Santa Ana Region, on January 19, 2001. The developer or builder shall comply with the requirements of the NPDES construction permit by developing and implementing a Storm Water Pollution Prevention Plan (SWPPP), approved by the Executive Officer of the Regional Water Quality Control Board, that specifies Best Management Practices (BMPs) to minimize pollutants in storm water runoff. During construction, the project would follow the specifications per the site's SWPPP.

5. WATER AND SEWER PLAN

The water and sewer facilities requirements identified below are based on correspondence from Eastern Municipal Water District (November 19, 1992) and the project civil engineer (June 1, 1993). The *MENIFEE NORTH* project proposes to phase sewer and water system improvements to serve the project area which would allow initial developments to proceed by connecting to existing facilities. As these developments proceed, generated capital from the collection of connection fees will provide funding for new facilities to serve the entire area. In addition, as development proceeds, *MENIFEE NORTH* will have the ability to form Community Facility/Assessment Districts to provide funding for additional improvements.

a. **Water Plan Description**

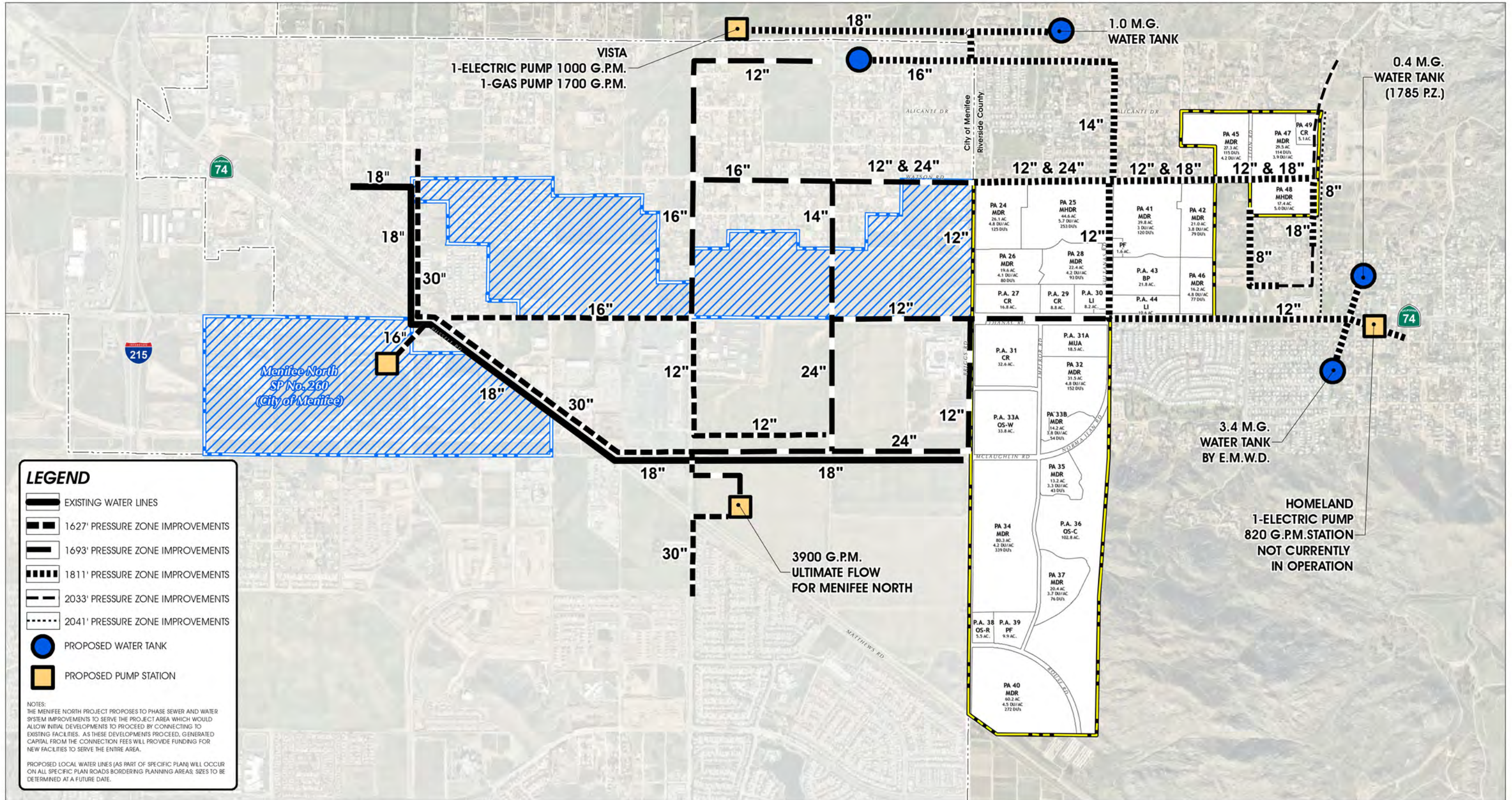
Domestic water will be supplied to the *MENIFEE NORTH* project area by Eastern Municipal Water District (EMWD). EMWD's facilities master plan indicates that system improvements should occur within the project area. The proposed backbone water distribution and storage system improvements for the project area are illustrated on Figure III-7, *Master Water Plan*. The plan for providing water service to the area is based on EMWD's regional planning effort and therefore is compatible with the District's current planning concepts. District water system improvements include approximately 12 million gallons of storage with water lines ranging in diameter from 12" to 30". Pumping improvements include approximately 5,500 gallons per minute of new capacity as well as reworking and/or automation of the Ellis, Homeland and Vista pumping plants.

The project is included within four existing water pressure zones: 1693' zone, 1785' zone, 1811' zone, and 2033' zone. Development is anticipated to occur within Planning Areas 24, 25, ~~and 45, 47, 48, and 49~~ by connecting to the 1811'/1785' and the 2033' zone systems with minimal off-site water line improvements.

Planning Areas 33 and 34 will be served by constructing additional pumping capacity at the Vista Pump Station to increase the water supply into the 1811' zone. The storage for Planning Areas 33 and 34 will initially be provided by pressure reduction into the 1693' pressure zone from the 1811'/1785' pressure zone.

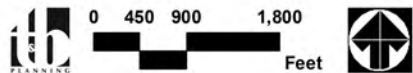
Currently, the 1693' zone has no storage. Water is pumped from the 1697' zone through the 1693' zone and into the 1811' zone. Currently, there is approximately one million gallons of storage in the 1811' zone and 0.4 million gallons of storage in the 1785' zone. In EMWD's master plan, the 1811' zone and the 1785' zone merge and continue providing service as the 1811' zone. This will occur once EMWD constructs a 3.4 million gallon storage tank southeasterly of Juniper Flats Road and Highway 74, along with other facilities necessary to fill this tank, providing available storage capacity for new connections.

It is anticipated that EMWD will require the project to construct reclaimed water lines on-site so that when the regional system is complete, the project can ultimately utilize reclaimed water for certain types of irrigation.



Source(s): ESRI, Near Map Aerial (2021), RCTLMA (2021)

FIGURE III-7



MASTER WATER PLAN

III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

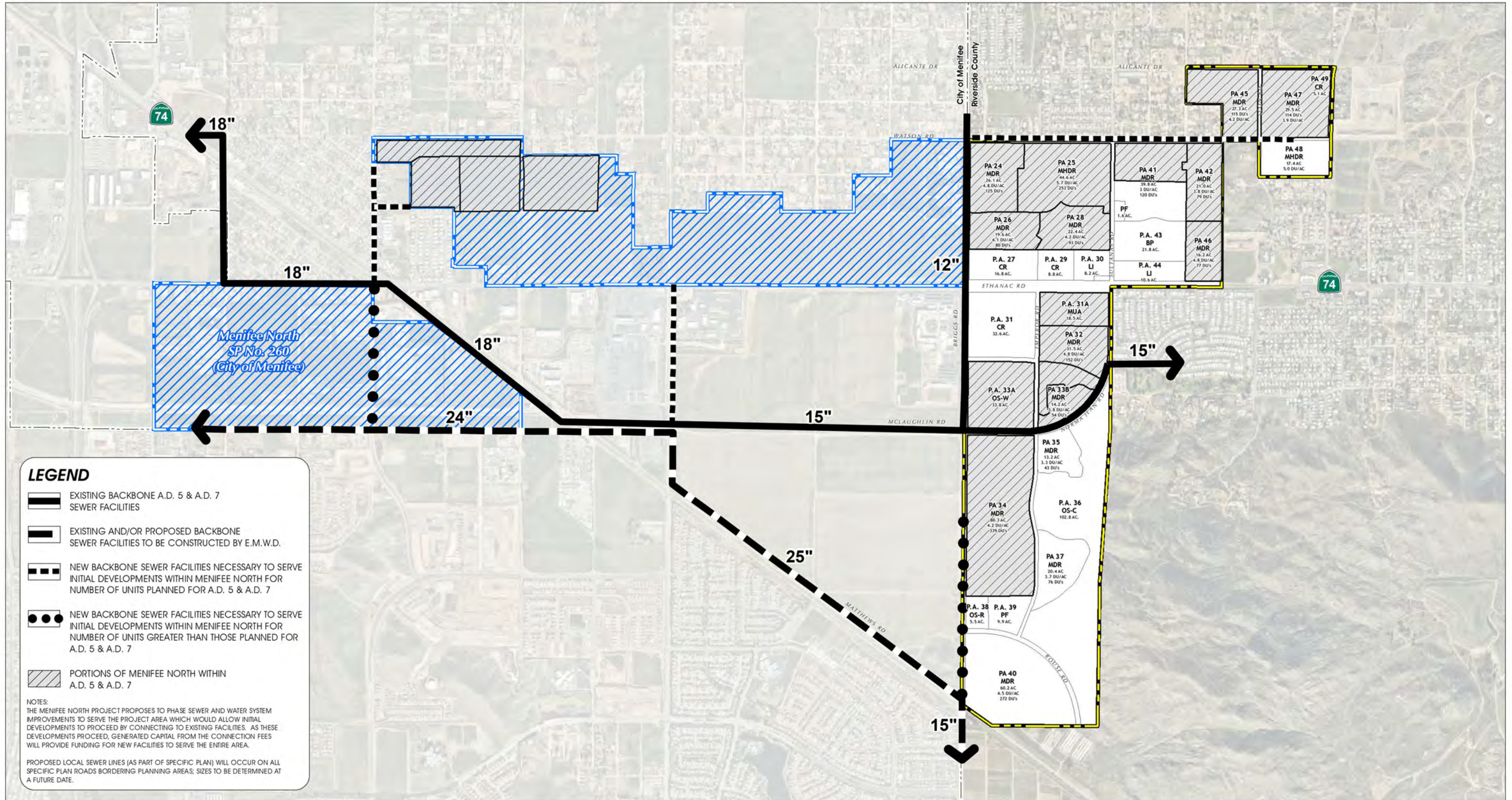
b. Sewer Plan Description

Collection and treatment of wastewater will be provided by Eastern Municipal Water District (EMWD). Portions of the *MENIFEE NORTH* project are located within EMWD Assessment Districts Nos. 5 (Romoland AD) and 7 (Homeland AD). These portions of the project site are eligible to connect to the AD funded facilities for wastewater service. However, due to the project's service demand above that which was anticipated at the time AD Nos. 5 and 7 were formed, some improvements to these systems would be necessary to provide an adequate level of service.

In order for the portions of the project area not located within AD Nos. 5 or 7 to receive sewer service, system improvements will be necessary. EMWD's wastewater facilities master plan describes a conceptual layout of gravity-flow sewer lines that would accomplish the required service. The *MENIFEE NORTH* project would be required to design and construct master-planned facilities which would allow for a system of sewers located within public road right-of-ways which are capable of conveying all on-site generated flow by gravity. Backbone wastewater system improvements necessary to provide an adequate level of service to *MENIFEE NORTH* are illustrated on Figure III-8, *Master Sewer Plan*. For residential areas located within AD Nos. 5 and 7, sewer lines will be constructed to join the existing AD funded sewer facilities to developing areas up to the AD planned densities. Densities greater than planned for the ADs will pay connection fees and construct facilities to join the District funded 24" and larger trunk sewer main that will connect to the expanded Perris Valley Regional Water Reclamation Facility.

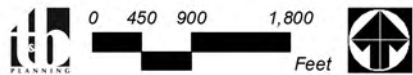
c. Water and Sewer Plan Development Standards

- 1) All water and sewer lines shall be placed underground.
- 2) All water and sewer lines shall be designed per the Eastern Municipal Water Districts requirements.
- 3) The infrastructure system will be installed to the requirements of the County Building and Safety Department.
- 4) Water and sewage disposal facilities shall be installed in accordance with the requirements and specifications of the Riverside County Health Department.
- 5) The project developer shall submit information which describes estimates of the project's reclaimed water demand, and landscape/irrigation conceptual plans to EMWD for review. At the time of EMWD's review, a determination shall be made regarding requirements for reclaimed water use and system improvements by the subject project.



Source(s): ESRI, Near Map Aerial (2021), RCTLMA (2021)

FIGURE III-8



MASTER SEWER PLAN

III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

6. OPEN SPACE AND RECREATION PLAN

a. Open Space and Recreation Plan Description

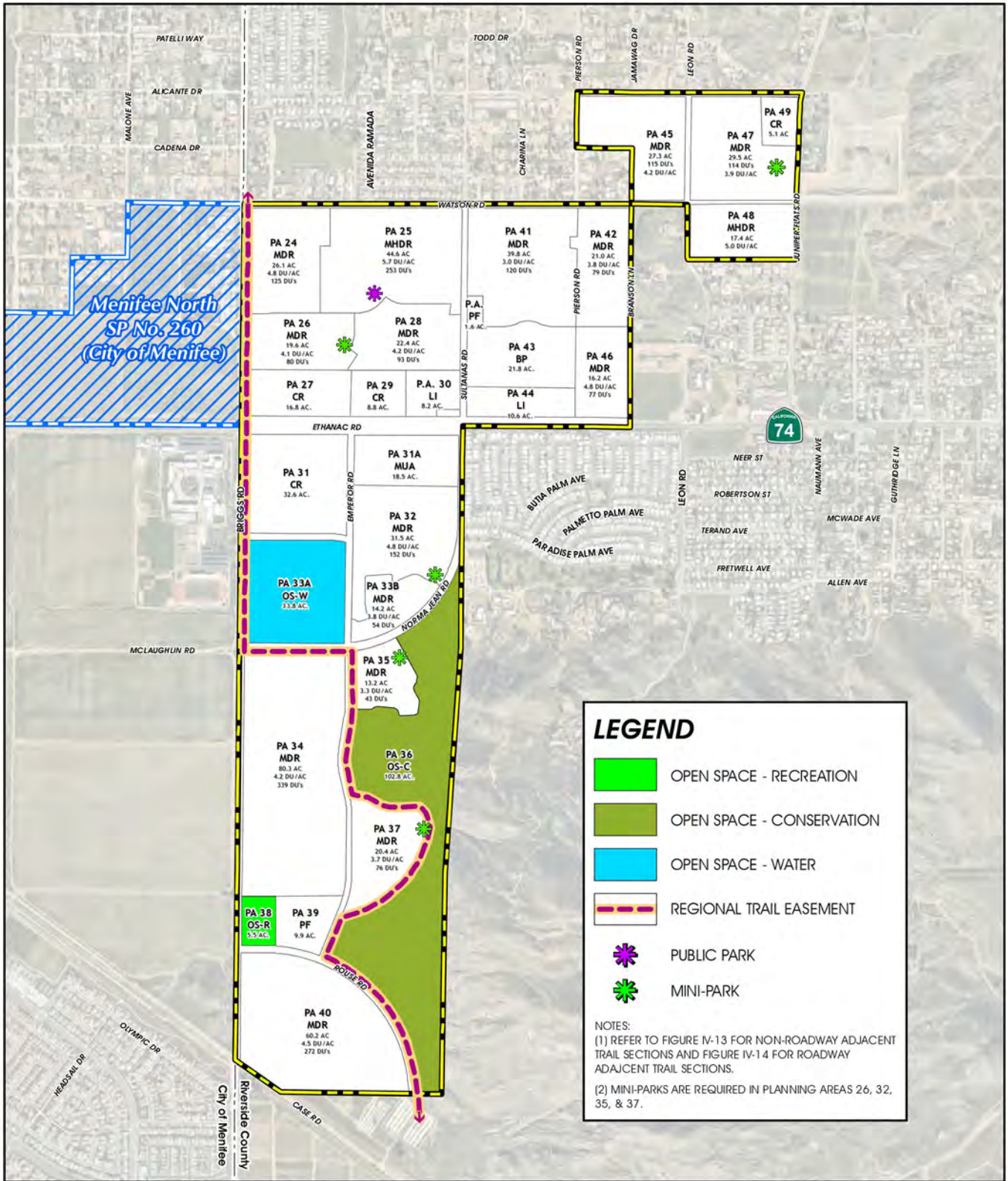
The *MENIFEE NORTH* project provides for an open space and recreation program which offers recreational opportunities in which all members of the community can participate. The program incorporates many diverse elements in a coordinated, cohesive plan that interrelates with and links the various neighborhoods of the community and certain destination points, such as an elementary school, neighborhood parkland, open spaces and shopping facilities. Recreational opportunities vary from passive (i.e. undeveloped open space), to active (i.e. community parks and mini-parks). Varying types and degrees of activities will be available which will provide residents with the opportunity to take quiet "walks in the park," participate in social gatherings, and participate in active outdoor informal recreational activities such as hiking and biking. Figure III-9, *Open Space and Recreation Plan*, depicts the various open spaces and recreational areas that are proposed for *MENIFEE NORTH*. The elements and acreages of the *Open Space and Recreation Plan* are summarized in Table III and discussed below.

~~The adopted MENIFEE NORTH Specific Plan Amendment No. 2 provided 29.4 acres of neighborhood parkland as well as trails and other open space. However, since the adoption of Amendment No. 2, the City of Menifee incorporated, and annexed the portion of MENIFEE NORTH located west of Briggs Road, which included 23.9 acres of the Specific Plan's 29.4 acres of recreational facilities.~~

**Table III
OPEN SPACE AND RECREATION PLAN SUMMARY**

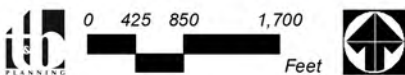
	ACREAGE
Parks	
• Community Park (Planning Area 38).....	5.5
• Mini-Parks (Portions of Planning Areas 26, 32, 35, 37, and 47).....	13.4
• Public Park (within a portion of Planning Area 25).....	4.9
School Recreation Facilities.....	—*
SUBTOTAL	15.7
Natural Open Space.....	102.8
Community Trail.....	—
Landscaped Parkways.....	—
OVERALL OPEN SPACE AND RECREATION OPPORTUNITIES	121.8

*School recreation facilities will be available for community use during non-school hours at the discretion of the School District.



Source(s): ESRI, Near Map Aerial (2021), RCTLMA (2021)

FIGURE III-9



OPEN SPACE AND RECREATION PLAN

III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

- **COMMUNITY PARK:** The *MENIFEE NORTH* Specific Plan includes one community park in Planning Area 38, and is designated as Open Space-Recreation. This 5.5-acre park is located in the southern portion of the community and is designed to offer a variety of active recreational opportunities to *MENIFEE NORTH* residents. The park in Planning Area 38 is located adjacent to the elementary school site, providing an excellent opportunity for sharing facilities. Proposed park amenities include picnic areas, tot lots, sport fields and courts, and barbecue facilities, as well as parking areas and restrooms.
- **PUBLIC PARK:** One (1) 4.9-acre public park is located within residential Planning Area 25 and is designed to offer a variety of recreational opportunities to *MENIFEE NORTH* residents and residents of the surrounding communities. Park amenities may include a sports field, basketball court, tot lots, shaded structures, and walking paths. This park will be owned and maintained by the Valley-Wide Recreation and Park District.
- **MINI-PARKS:** Five mini-parks, totaling approximately 139 acres are proposed within the *MENIFEE NORTH* community. The mini-parks, varying in size from 0.2 acres to 5 acres are proposed within Planning Areas 26, 32, 35, 37, and 47. A Mini-Park may be provided in Planning Area 47, to be determined with implementing project(s). These parks will function as central gathering places for the neighborhoods in which they are located, adding to the overall amenity package for *MENIFEE NORTH*. The mini-parks may be either public or private.
- **NATURAL OPEN SPACE:** The *MENIFEE NORTH* community provides for 102.8 acres of natural open space. The open space—represented by Planning Area 36—is located on the southeastern portions of the site. The open space may allow for some passive recreational activities, depending upon the nature of the vegetation and terrain.
- **REGIONAL TRAIL AND LANDSCAPED PARKWAYS:** A regional trail will be provided in Planning Area 36 and along designated project roadways. Landscaped parkways will be incorporated within the streetscapes of major project roads. These elements will provide additional open space for pedestrian use.
- **SCHOOL RECREATION FACILITIES:** One elementary school is proposed for *MENIFEE NORTH*. Recreational facilities developed on the school sites will be available for community use during non-school hours, at the discretion of the School District. Typical recreational equipment and sports fields which may be provided by the School District include playground equipment, basketball courts, tennis courts, volleyball courts, softball fields, soccer fields and football fields.

The Specific Plan limits reflected in Amendment No. 4 provide for 15.3 acres of dedicated, neighborhood parkland, which include a 5.5-acre community park in Planning Area 38, a 4.9-acre Public Park in Planning Area 25, and 5.3-acres of ~~pocket-mini~~ parks in Planning Areas 26, 32, 35, and 37. In addition, recreation facilities at the school in Planning Area 39 will be available to residents, and passive recreation opportunities are provided in Planning Area 36 (which includes a regional trail).

Riverside County's policy implementing the ~~State-Quimby-Ordinance~~ Quimby Act (regulating parkland requirements) requires that 5.0 acres of parkland be provided for each 1,000 residents. Using the County's standard of ~~2,593.1~~ persons per single-family dwelling unit (attached garage) and 2,0798 dwelling units, *MENIFEE NORTH* is estimated to generate a population of ~~5,3826,445~~. As depicted below in Table IIIA -

Quimby Act Requirements, ~~the project would require 26.9 acres of parkland, the 2,079 dwelling units located within the MENIFEE NORTH Specific Plan generate the need for 32.2 acres of parkland (using the Valley Wide Recreation and Park District population number of 3.1 persons per household and 5 acres per 1,000 persons). However, MENIFEE NORTH provides 23.8 acres of parkland, a shortfall of 8.4 acres of parkland, as a result of the incorporation of the City of Menifee in 2008, the annexation of the area west of Briggs Road by the City of Menifee removed 23.9 acres of the Specific Plan's 29.4 acres of recreational facilities. Substantial Conformance No. 2 increases the park acreage within the County portion of the SP from 10.6 to 14.0 acres.~~

**Table IIIA
QUIMBY ACT REQUIREMENTS**

LAND USE	SPECIFIC PLAN RESIDENTIAL DENSITY	ORDINANCE 460 HOUSEHOLD GENERATION RATE	NO. OF DWELLING UNITS	TOTAL RESIDENTS	QUIMBY ACREAGE REQUIRED
Single Family Detached	Medium and Medium-High	3.12 59	2,079 8	5,3826,445	26.932.2
TOTAL				5,3826,445	26.932.2

b. Open Space and Recreation Plan Development Standards

- 1) A community park will be provided for the benefit of all residents within the community.
- 2) At a minimum, the community park will contain the following facilities/equipment: Picnic area(s), tot lot(s), two sport fields or courts, restrooms and parking areas.
- 3) The community park (Planning Area 38), Public Park (Planning Area 25), natural open space (Planning Area 36), the regional trail and landscaped parkways will be publicly owned and maintained. Possible maintenance mechanisms include CSA 146, Valley-Wide Recreation and Park District and the formation of a new Community Service District. The maintenance mechanism shall be selected at the time that the first implementing development application is submitted.
- 4) The mini-parks may either be public or private. If they are public, they shall be owned and maintained as described in item 3) above. If they are private, a neighborhood homeowners' association shall be formed for ownership and maintenance.
- 5) All recreational facilities will be landscaped, and where necessary, irrigated in a manner that is conducive to the type of plant material and landscape setting.
- 6) All recreational facilities will provide parking in accordance with Riverside County standards.
- 7) Landscaping within recreation and open space areas will be further governed by the Development Standards contained with this Specific Plan (Section III.B.7.) and the Design Guidelines contained within this Specific Plan (Section IV).

7. GRADING PLAN

a. Grading Plan Description

The *MENIFEE NORTH* Specific Plan grading is tailored to the existing topography of the project site. It is intended that the proposed plan be sensitive to and reflect natural landforms where possible, while incorporating project requirements for drainage and other infrastructural improvements so that residential, commercial, industrial and recreational uses are accommodated. (See Figure III-10, *Grading Concept*.) Due to the location of existing infrastructure in the vicinity of the Specific Plan, it may be necessary to utilize a grading design for a particular implementing project which modifies the landform substantially to allow connection to the closest existing infrastructure facilities. Infrastructure which relies on the elevation of each residential or commercial parcel to provide adequate service, such as adequate domestic water pressure, or which relies on relative elevations to reach existing infrastructure via gravity (sewer and storm water) should be considered as appropriate practical reasons why certain typical grading practices cannot be met.

It is always the goal of the Conceptual Grading Plan to achieve a balanced site overall. However, within the Specific Plan, there may be instances related to particular implementing projects where this may not be possible given the constraints of the implementing project. Where a balanced site cannot be achieved, the CEQA document for that implementing project should evaluate the impacts of that project related to grading and earthwork balance. According to an earthwork quantity take-off conducted by the project engineer, it appears that the project site will balance and will not require import or export.

b. Grading Plan Development Standards

- 1) All grading activities shall be in substantial conformance with the overall Grading Concept (Figure III-10) and shall implement any grading related mitigation measures.
- 2) Prior to any development within any area of the Specific Plan boundaries by an implementing project, an overall Conceptual Grading Plan for that implementing project the portion in process shall be submitted for Planning Department approval.
 - a. The Conceptual Grading Plan for each such area shall be used as a guideline for subsequent detailed grading plans for individual stages of development within that area, and shall include:
 - i. Techniques employed to prevent erosion and sedimentation during and after the grading process;
 - ii. approximate time frames for grading; identification of areas which may be graded during higher probability rain months (January through March);
 - 2)iii. and preliminary pad and roadway elevations.
- 3) All streetsStreets shall have a gradient not exceeding 15%.
- 4) Graded slopes over 30' in height shall be oriented to minimize visual impacts to surrounding areas, where practical, given the need to provide adequate domestic water pressure, or connect to the closest available gravity sewer main (if possible) or connect to the closest available storm water management facilities.
- 5) The overall slope, height and grade of any cut and fill slope shall be developed in concert with the existing natural contours and scale of the natural terrain of a particular site, where practical, given the need to provide adequate domestic water pressure, or connect to the closest available gravity sewer main (if possible) or connect to the closest available storm water management facilities.-

- 6) The toes and tops of all slopes higher than ~~ten (10)~~35 (35) feet shall be rounded with curves with radii designed in proportion to the total height of the slope, where drainage and stability permit such rounding.
- 7) Cut or fill slopes over 35 feet in height and exceeding one hundred (100) feet in horizontal length, if any, shall be graded to meander the toe and top of the slope.
- 8) Prior to initial grading activities, a detailed soils report and geotechnical study shall be prepared to analyze on site soils conditions and slope stability and will include appropriate measures for erosion and dust.
- 9) All grading plans shall include an erosion control plan that shall address the techniques and measures to prevent erosion and sedimentation as well as eliminate source pollutants during and after the grading process. In accordance with the NPDES requirements a Stormwater Pollution Prevention Plan (SWPPP) shall be prepared for the project and implemented during the construction process.
- 10) Where cut and/or fill slopes are created higher than five (5) feet in height, detailed landscaping and irrigation plans shall be approved by the County of Riverside, in accordance with Ordinance No. 457.
- 11) Retaining walls required to implement the grading shall be reviewed and approved by the County of Riverside and shall be permitted separately from the grading plans.
- 12) The applicant shall be responsible for maintenance and upkeep of all planting and irrigation systems until those operations are the responsibility of other parties.
- 13) Brow ditches, terrace drains, or other minor swales shall be lined with natural erosion control materials or concrete.
- 14) Unless otherwise approved by the County of Riverside Department of Building and Safety, all cut and fill slopes shall be constructed at inclinations of no steeper than two (2) horizontal feet to one (1) vertical foot.

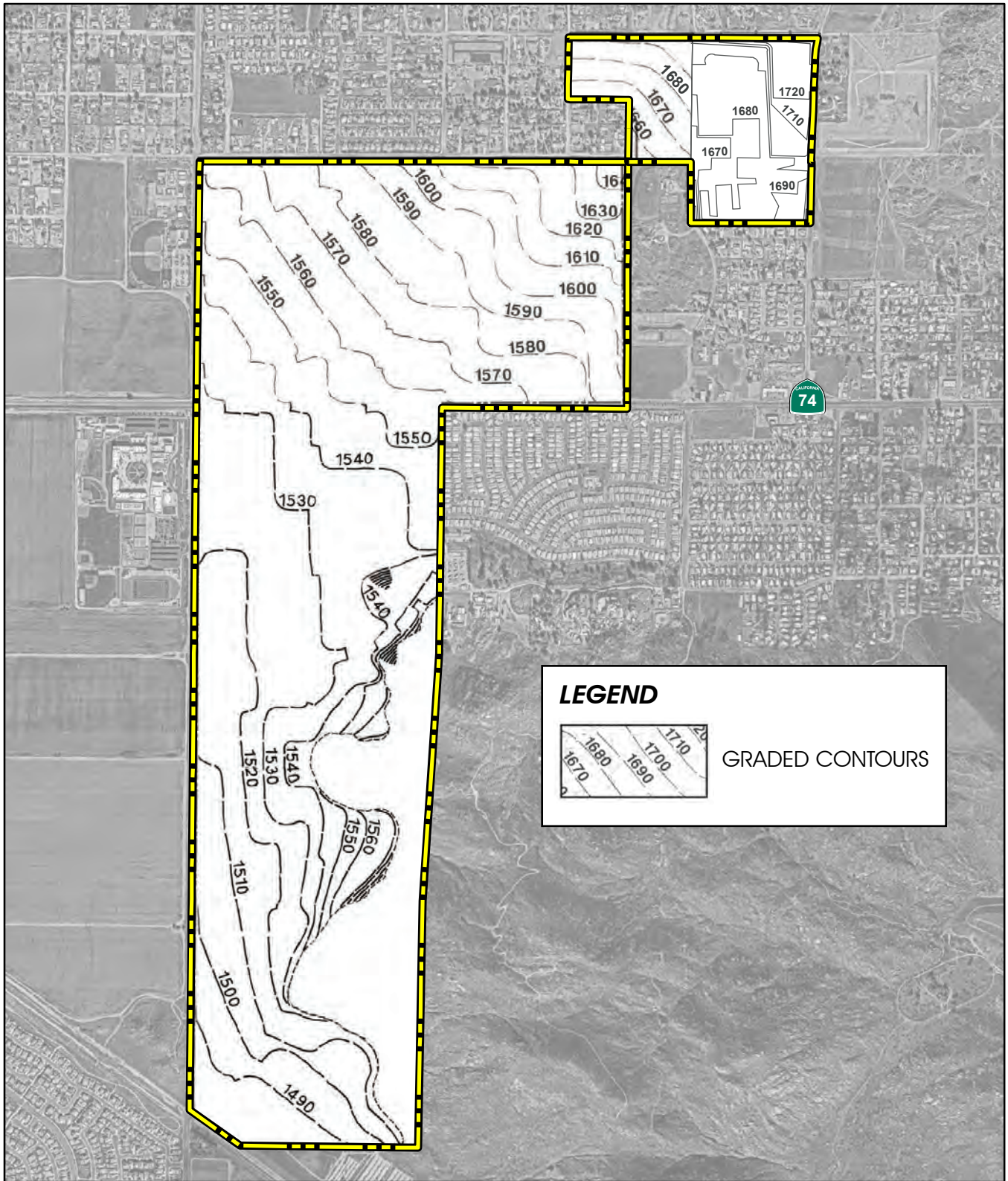
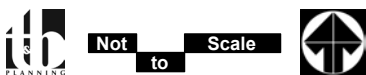


FIGURE III-10



Not to Scale

GRADING CONCEPT

III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

- | ~~158~~) Slopes exceeding ten (10) feet in vertical height shall be hydromulched, prior to final acceptance and prior to the beginning of the rain season (October-March).
- | ~~169~~) Prior to initial grading activities, a soils report and a geotechnical study shall be performed that further analyze on-site soil conditions and slope stability and include appropriate measures to control erosion and dust. (See Preliminary Geotechnical Investigations in Appendices.)
- | ~~170~~) Detailed grading plans shall be prepared prior to any on-site grading for each project or group of projects.
- | ~~184~~) All dwelling units shall have a minimum five (5) foot side setback and a minimum ten (10) foot rear setback from the toe of slopes higher than ten (10) feet. County Subdivision Ordinance 348 will be observed regarding setback requirements.
- | ~~192~~) Where cut and fill slopes are created higher than ten (10) feet, detailed landscaping and irrigation plans shall be submitted to the Planning Department prior to grading plan approval. The plans shall be reviewed for type and density of ground cover, shrubs and trees.
- | ~~2043~~) The applicant shall be responsible for maintenance and upkeep of all planting and irrigation systems until those operations are the responsibility of other parties.
- | ~~2144~~) Angular forms shall be discouraged. The graded form shall reflect natural terrain, where possible.
- | ~~2245~~) Potential brow ditches, terrace drains or other minor swales, determined necessary at future stages of project review, shall be lined with natural erosion control materials or concrete.
- | ~~2346~~) Grading work shall be balanced on-site whenever possible.
- | ~~2447~~) Graded but undeveloped land shall be maintained weed-free and planted with interim landscaping within 90 days of completion of grading, unless building permits are obtained.
- | ~~2548~~) Unless otherwise approved by the County of Riverside Department of Building and Safety, all cut and fill slopes shall be constructed at inclinations of no steeper than two (2) horizontal feet to one (1) vertical foot. The Grading Plan will reflect a contouring intended to control slope erosion.
- | ~~2649~~) Natural features such as significant rock outcrops shall be protected to the greatest extent feasible in the siting of individual lots and building pads.
- | ~~270~~) A grading permit shall be obtained from the County of Riverside, as required by the County Grading Ordinance 457, prior to grading. The project shall comply with all applicable provisions of Ordinance 457.
- | ~~284~~) If any historic or prehistoric remains are discovered during grading, a qualified archaeologist should be consulted to ascertain their significance. If human remains are discovered, work shall halt in that area and procedures set forth in the California Public Resources Code (Sec. 5097.98) and State Health and Safety Code (Sec. 7050.5) shall be followed, including notification to the County Coroner.
- | ~~292~~) Soil stabilizers shall be used to control dust as required by SCAQMD Rule 403.
- | ~~3023~~) The developer or builder for the *MENIFEE NORTH* Specific Plan shall be required, pursuant to requirements of the State Water Resources Control Board, to obtain a National Pollutant Discharge

Elimination System (NPDES) construction permit, prior to issuance of grading permits. There is a statewide General Permit applicable to most areas of the state; however, because *MENIFEE NORTH* is located in the San Jacinto Watershed, the project is subject to a special NPDES permit (No. CAG 618005) per Order No. 01-34 as adopted by the California Regional Water Quality Control Board, Santa Ana Region, on January 19, 2001. The developer or builder shall comply with the requirements of the NPDES construction permit by developing and implementing a Storm Water Pollution Prevention Plan (SWPPP), approved by the Executive Officer of the Regional Water Quality Control Board, that specifies Best Management Practices (BMPs) to minimize pollutants in storm water runoff. During construction, the project would follow the specifications per the site's SWPPP.

8. ILLUSTRATIVE LANDSCAPING PLAN

a. Landscaping Plan Descriptions

As illustrated on Figure III-11, *Conceptual Landscape Plan*, project landscaping will play an important role in maintaining project design themes, while emphasizing community continuity. This section of the Specific Plan provides a general description and development standards for the landscaping concept. Detailed landscaping information is provided in the Design Guidelines contained within this Specific Plan (Section IV.D.).

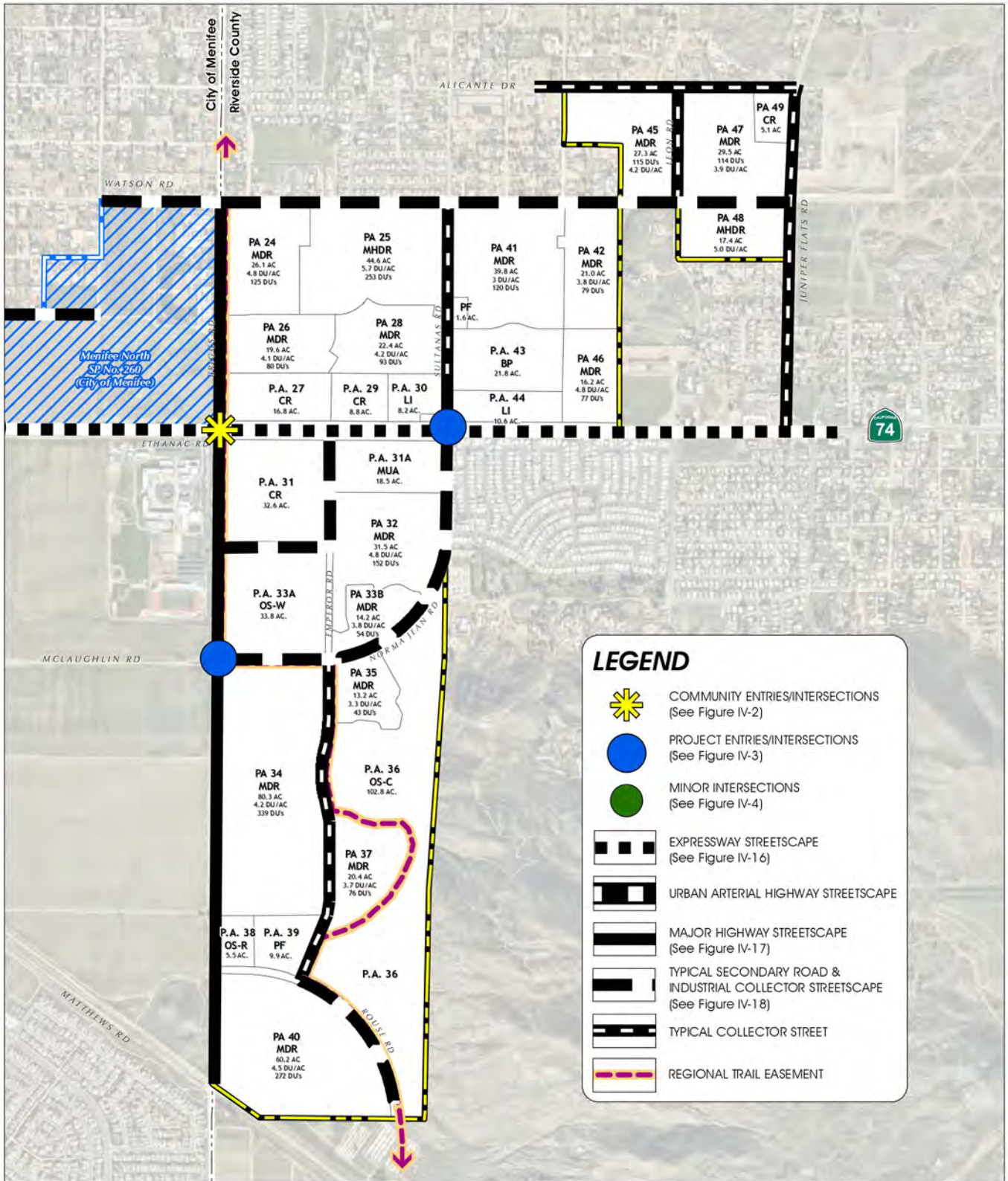
Entry monumentation will provide initial definition for the site, and will be viewed when approaching the site from any direction. Once within the site, entry monumentation will continue to be present at all key intersections. Monumentation will be developed in a hierarchical format which will provide initial community identification for each residential planning area, and the community as a whole.

Landscaping within the project itself will articulate community design elements at the commercial areas, business parks, parks, and schools and will define boundaries and transitions between differing land uses and easements. Individual neighborhoods and residential development enclaves will also be distinguished by varied planting themes. Special treatments including land use transition areas, will be provided between certain planning areas identified in the Planning Area Development Standards (Section III.B.).

Landscaping will be used to identify the hierarchy of the street system, from major access roads to interior residential streets, creating definite landscaped corridors. This is accomplished by careful considerations of the relationship between street and plant materials characteristics such as size, form, texture and color.

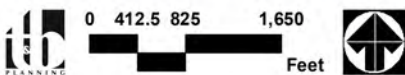
b. Landscaping Plan Development Standards

- 1) All detailed landscaping programs for planning areas and roadways shall be prepared by a qualified landscape architect for review by County staff and decision-making agencies.
- 2) Project entry statements shall be designed with landscaping and architectural treatments that project a high quality image for the mixed-use development.
- 3) The landscaping design for the site shall include turf, trees, shrubs and ground cover compatible with natural vegetation on-site, where feasible. Detailed landscaping design information is provided in the Section IV, *Design Guidelines* of this Specific Plan.
- 4) Special treatment areas shall be designed to provide definition to certain planning areas as identified in Section III.B.
- 5) Major entrance roads into the *MENIFEE NORTH* project shall have planted medians and landscaped shoulders to define the project's design concept. The introductory landscape theme shall include elements such as tree clustering to reinforce the project theme and character.



Source(s): ESRI, Near Map Aerial (2021), RCTLMA (2021)

FIGURE III-11



CONCEPTUAL LANDSCAPE PLAN

III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

- 6) Planted raised medians (according to Ordinance 461 Standard No. 113) may be established within any roadway right-of-way as long as access and safety criteria can be met as approved by the County Transportation Department.
- 7) Prior to recordation of any final subdivision map, improvement plans for the respective landscaped areas, or plans to mitigate an environmental impact for the stage of development, shall be submitted to the County Planning Department for approval. The improvement plans shall include, but not be limited to the following:
 - Final grading plan.
 - Irrigation plans certified by a landscape architect.
 - A landscape plan with seed mixes for mulching and staking methods; locations, type, size and quantity of plantings.
 - A hardscape plan with location, type and quantity of potential recreational amenities/facilities (in medium and medium-high density residential areas).
 - Fence treatment plans.
 - Special treatment/buffer area treatment plans.
- 8) The master developer shall be responsible for maintenance and upkeep of all slope plantings, common landscaped areas, and irrigation systems until such time as these operations are the responsibility of other parties.
- 9) At the time of recordation of any tentative subdivision which contains a common greenbelt or open space area, the subdivision shall have those common areas conveyed to the master property-owners' association or appropriate public maintenance agency.
- 10) The landscaping plan shall reflect the following water conservation methods, whenever possible: landscape with low water-using plants, wherever feasible; group plants of similar water use to reduce over-irrigation of low water-using plants; use mulch extensively, as mulch applied on top of soil will improve the water-holding capacity of the soil by reducing evaporation and soil compaction; install efficient irrigation systems that minimize runoff and evaporation and maximize the water that will reach plant roots. Drip irrigation, soil moisture sensors, and automatic irrigation systems are a few methods of increasing irrigation efficiency.
- 11) Front yard landscaping shall be provided on all lots smaller than one-half acre. Typical plans will be submitted during tract map processing.
- 12) For further Landscape Development Standards, please refer to Section IV.D., Design Guidelines.

9. PUBLIC FACILITY SITES PHASING PLAN

a. Public Facility Phasing Description

In order to ensure timely development of public facilities, a phasing plan has been prepared for the community parks (Planning Area 38), Public Park (Planning Area 25), the elementary schools (Planning Area 39) , and the mini-parks within Planning Areas 26, 32, 35, 37, and 47. The public facility sites phasing plan is depicted in Figure III-12.

Public facility construction shall be phased as provided by the Public Facilities Phasing Table (Table IV), below:

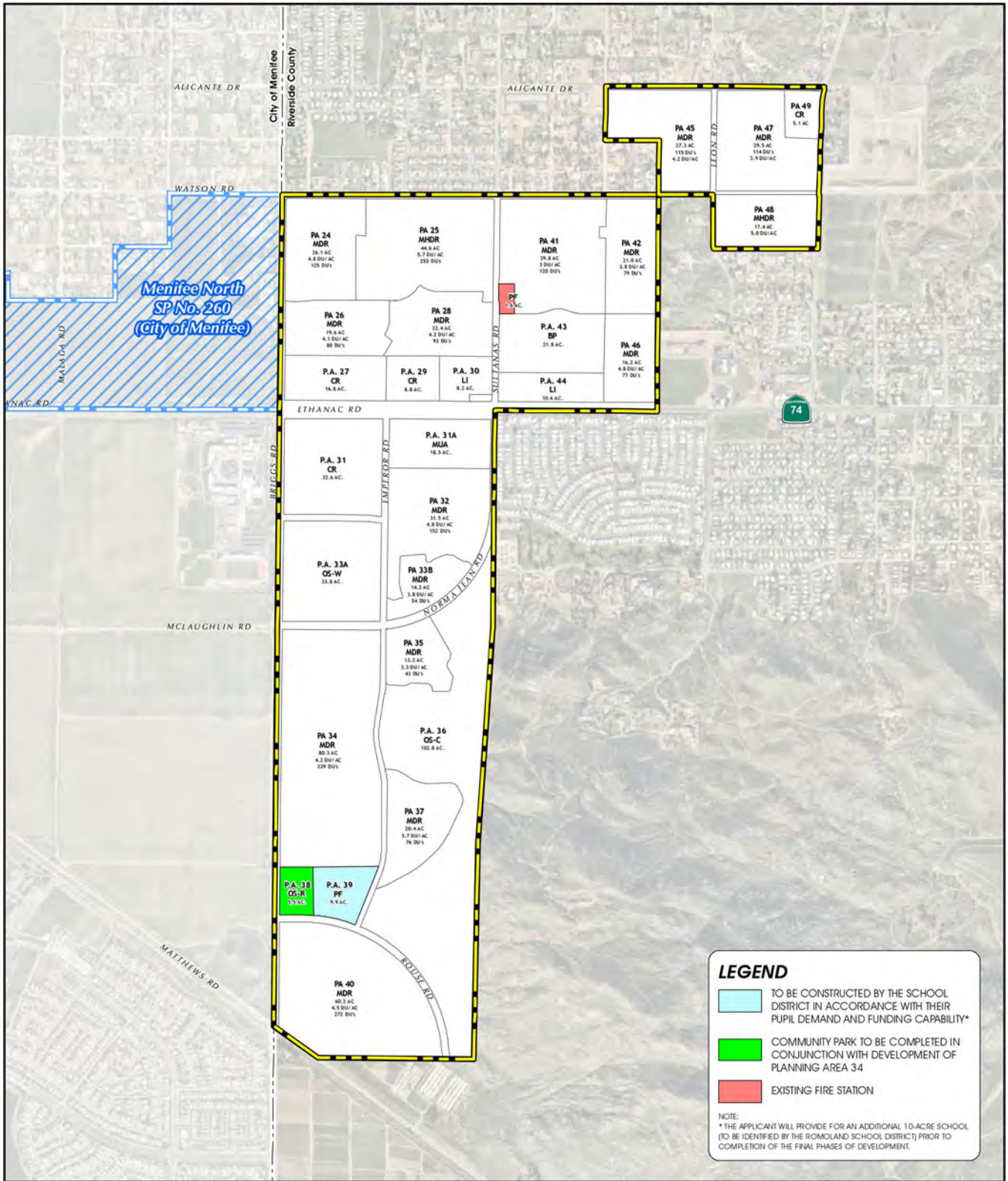
**Table IV
PUBLIC FACILITIES PHASING**

PLANNING AREA	PUBLIC FACILITY	ACREAGE	MILESTONES AND REQUIREMENTS
25 (portion)	Public Park	4.9	To be completed during Phase I, prior to issuance of the 280th building permit in Planning Areas 24 and 25.
26 (portion)	Mini-Park	0.2	To be completed during Phase I, prior to issuance of the 60th building permit in Planning Area 26.
32 (portion)	Mini-Park	4.0 *	To be completed during Phase I, prior to issuance of the 25th building permit in Planning Area 32.
35 (portion)	Mini-Park	0.4	To be completed during Phase I, prior to issuance of the 21st building permit in Planning Area 35.
38	Community Park	5.5	To be completed during Phase I in conjunction with the development of Planning Area 34.
37 (portion)	Mini-Park	0.2	To be completed during Phase III, prior to issuance of the 23rd building permit in Planning Area 37.
47 (portion)	Mini-Park	3.1*	If determined by implementing project(s), to be completed during Phase I, prior to the issuance of the 85 th building permit
39	Elementary School	9.9	To be constructed by the School District in accordance with their pupil demand and funding capabilities.
	Fire Station	1.6	Existing.

** The acreage of this mini-parks an approximation.*

b. Public Facility Phasing Standards

- 1) Improvement plans for mini-park sites shall accompany the first development application for the Planning Area in which the park is contained.
- 2) Improvement plans for the 5.5-acre community park (Planning Area 38) shall accompany the first development application for Planning Area 34 or 40, whichever occurs first.
- 3) Improvement plans for the Public Parks within Planning Areas 25 and 47 shall accompany the first development application for Planning Areas 25 and 47, ~~(if constructed)~~, respectively.



Source(s): ESRI, Near Map Aerial (2021), RCTLMA (2021)

FIGURE III-12



PUBLIC FACILITY SITES PHASING PLAN

III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

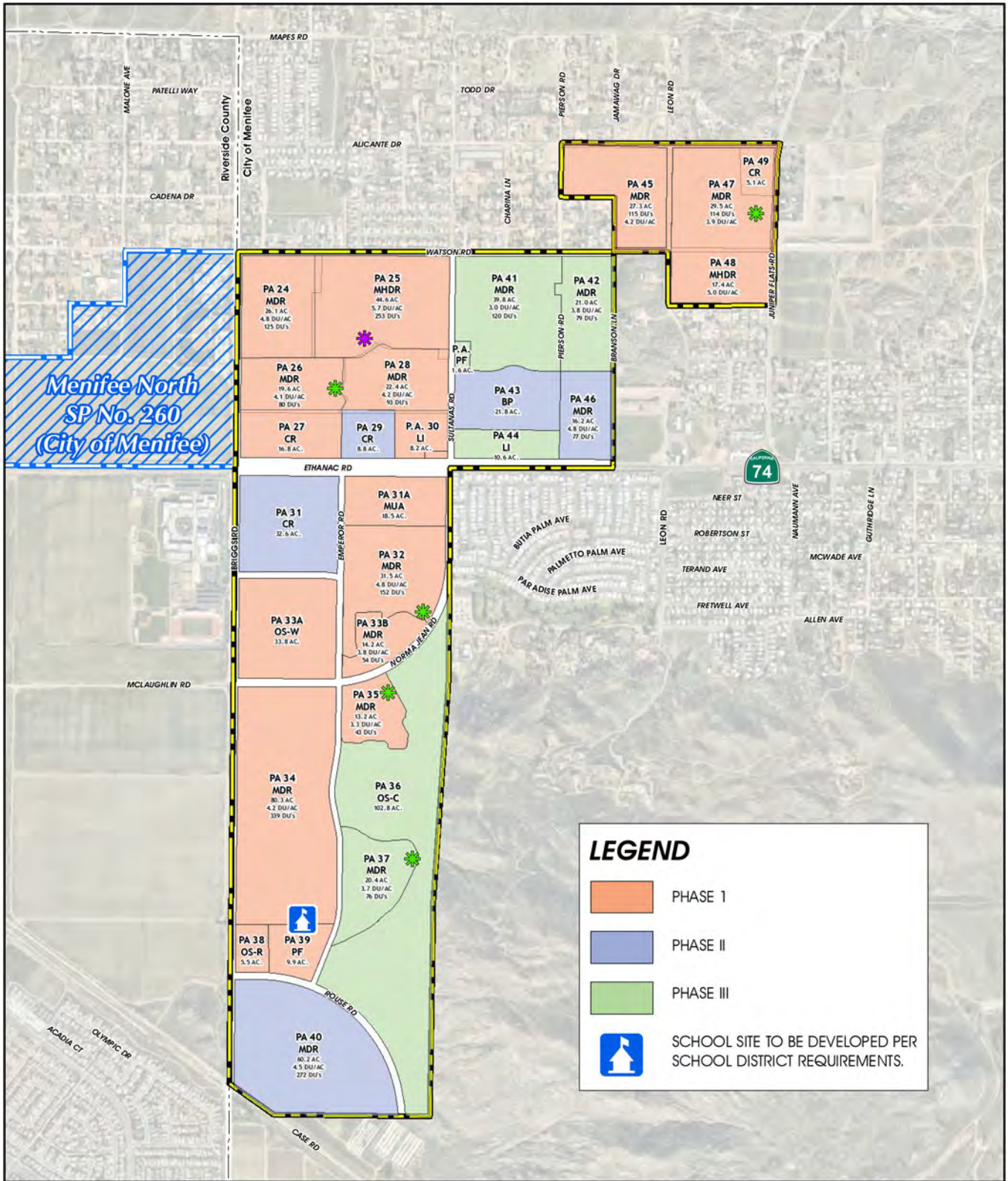
10. PROJECT PHASING PLAN

a. Project Phasing Plan Description

MENIFEE NORTH has three phases to be developed in response to market demands and according to a logical and orderly extension of roadways, public utilities and infrastructure. Figure III-13, *Phasing Plan*, is provided as a visual guide to how development is anticipated to proceed. Table V, *Phasing Summary*, outlines the acreages and number of dwelling units of each phase.

b. Project Phasing Standards

- 1) Prior to recordation of any final subdivision map, improvement plans for the respective landscaped areas, or plans to mitigate an environmental impact for that stage of development, shall be submitted to the County Planning Department for approval. The improvement plans shall include, but not be limited to the following:
 - Final grading plan.
 - Irrigation plans certified by a landscape architect.
 - A landscape plan with seed mixes for mulching, staking methods and locations, type, size and quantity of plant materials.
 - Fence treatment plans.
 - Special treatment/buffer area treatment plans.
- 2) Each planning area shall include development of common open space areas, infrastructure and adjacent landscape development zones.
- 3) Construction of the development permitted hereby, including recordation of final subdivision maps, may be done progressively in stages, provided adequate vehicular access is constructed for all dwelling units in each stage of development and further provided that such phase of development conforms substantially with the intent and purpose of the Specific Plan Phasing Program.
- 4) Prior to the first subdivision map approval in each planning area, the applicant shall provide evidence to the Planning Department of progress towards completion of the development standards outlined for that planning area.
- 5) The phasing sequence described herein is conceptual based on current market demand. Certain planning areas may be developed out of the expected sequence, provided the required infrastructure and services are available at the time of development.



Source(s): ESRI, Near Map Aerial (2021), RCTLMA (2021)

FIGURE III-13



CONCEPTUAL PHASING PLAN

III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

**TABLE V
PHASING PLAN**

LAND USE	PLANNING AREA	ACRES	MAXIMUM DWELLING UNITS
PHASE I			
Medium Density Residential	24	26.1	125
Medium-High Density Residential	25	44.6	253
Medium Density Residential	26	19.6	80
Commercial Retail	27	16.8	—
Medium Density Residential	28	22.4	93
Light Industrial	30	8.2	—
Mixed Use/Planning Area	31A	18.5	—
Medium Density Residential	32	31.5	152
Medium Density Residential	33B	14.2	54
Medium Density Residential	34	80.3	339
Medium Density Residential	35	13.2	43
Open Space - Recreation	38	5.5	—
Medium Density Residential	45	27.3	115
Medium Density Residential	47	29.5	114 2
Medium-High Density Residential	48	17.4	87 8
Commercial Retail	49	5.1	—
SUBTOTAL - PHASE I		380.2	14554
PHASE II			
Commercial Retail	29	8.8	—
Commercial Retail	31	32.6	—
Medium Density Residential	40	60.2	272
Business Park	43	21.8	—
SUBTOTAL - PHASE II		123.4	272
PHASE III			
Medium Density Residential	37	20.4	76
Medium Density Residential	41	39.8	120
Medium Density Residential	42	21.0	79
Light Industrial	44	10.6	—
Medium Density Residential	46	16.2	77
SUBTOTAL - PHASE III		108.0	352
OTHER LAND USES			
Public Facility (School)	39	9.9	—
Major Roads	—	74.8	—
Easements/Existing Uses	—	0.6	—

LAND USE	PLANNING AREA	ACRES	MAXIMUM DWELLING UNITS
Public Facility (existing Fire Station)	—	1.6	—
Open Space-Conservation	—	102.8	—
Open Space-Water	33A	33.8	—
SUBTOTAL - OTHER LAND USES		<u>225.8223.5</u>	—
PROJECT GRAND TOTAL		<u>836.8835.1</u>	<u>2,083,207.9</u>

11. COMPREHENSIVE MAINTENANCE PLAN

Successful operation of maintenance districts and associations are important in maintaining quality in the project area. It is anticipated that maintenance responsibilities for common project facilities will be divided among a Master Maintenance Organization, Neighborhood Associations or similar financing mechanisms. The decision regarding the maintenance mechanism will be made at a future stage of project design and review in concert with County agencies.

a. Master Maintenance Organization

Common areas identified in the Specific Plan shall be owned and maintained by a permanent public or private master maintenance organization, to assume ownership and maintenance responsibility for all common recreation, open space, circulation systems and landscaped areas (including those in commercial areas). Areas of responsibility would include the community parks, the community center, parkway landscaping medians, entry monuments, open space and the community trail. The following are possible maintenance mechanisms:

- 1) CSA 146 - is not currently empowered to handle landscape maintenance. The portion of the CSA covering *MENIFEE NORTH* could be empowered to meet the project's maintenance needs.
- 2) Valley-Wide Recreation and Park District - could maintain all project level facilities. The District currently performs similar functions in Hemet and in the Meniffee project south of *MENIFEE NORTH*. *MENIFEE NORTH* is currently within the Valley-Wide sphere of influence but is not within or abutting the current District boundaries. Under the current procedures and regulations, annexation of the project site and certain intervening properties would be necessary.
- 3) Community Service District (CSD) - A new CSD could be formed, designed specifically for the *MENIFEE NORTH* project.

b. Residential Neighborhood Associations

In certain residential areas of the project, smaller associations may be formed to assume maintenance responsibility for common areas and facilities that benefit only residents in those areas. Potential private recreation centers, mini-parks, common open space areas, and potential private roadways exemplify facilities that may be under the jurisdiction of a neighborhood association.

c. Commercial Retail/Business Park/Light Industrial/Mixed Use Planning Areas

Commercial Retail, Business Park, Light Industrial, and Mixed Use Planning Areas may have their own private associations. If no association is formed, a common maintenance charge will be assessed to cover common area maintenance. Maintenance for these planning areas also may be assumed by individual property owners.

d. Project Roadways

All public project roadways and private streets will be designed and constructed to standards acceptable to the County. All public roads will be entered into the County system of roads for operation and maintenance, as approved by the Riverside County Board of Supervisors. Private roadways, if any, will be maintained by a private homeowners' association.

e. School Sites

It is anticipated that maintenance responsibilities for one elementary school site in Planning Area 39 will be overseen by the governing School District.

B. PLANNING AREA DEVELOPMENT STANDARDS

Development standards for *MENIFEE NORTH* have been established at three levels: *General Development Provisions*, which were addressed in Section III; *Design Guidelines*, which are provided in Section IV; and *Planning Area Development Standards* to which this section is devoted. In addition, the standards of the Specific Plan Zone applicable to *MENIFEE NORTH* shall apply.

Planning areas were selected on the basis of logical, separate units of development. Criteria considered in this process included uniformity of use as it pertains to zoning, relationship to adjoining product and surrounding topography.

The planning area graphics for this section (Figures III-14 through 26) were derived from the *Conceptual Landscape Plan* (Figure III-11). The site plans depicted herein are only conceptual in nature. Although development may conform closely to some elements of the illustrative plans provided in Section IV it is anticipated that actual lotting will not be determined until the tract map stage.

A Specific Plan Zoning Ordinance was prepared and submitted separately from this Specific Plan document. The zoning provisions within that Ordinance establish use restrictions for each planning area. The zoning provisions should be used in conjunction with the planning standards for each respective planning area.

Development standards and corresponding figures for Planning Areas 1 through 23 have been deleted from Section III.B (Specific Plan, Planning Area Development Standards) of this document because these Planning Areas are outside the jurisdiction of Riverside County. As a result, subsections B.1 through B.23 have been deleted, and Planning Area development standards begin with subsection B.24.

Figure sequencing has also been updated to account for the removal of Planning Areas 1 through 23.

24. PLANNING AREA 24: MEDIUM DENSITY RESIDENTIAL

a. Descriptive Summary

Planning Area 24, as depicted in Figure III-14, provides for development of 26.1 acres of Medium Density Residential uses with minimum lot sizes of 5,000 square feet. A maximum total of 125 dwelling units are planned at a target density of 4.8 du/ac.

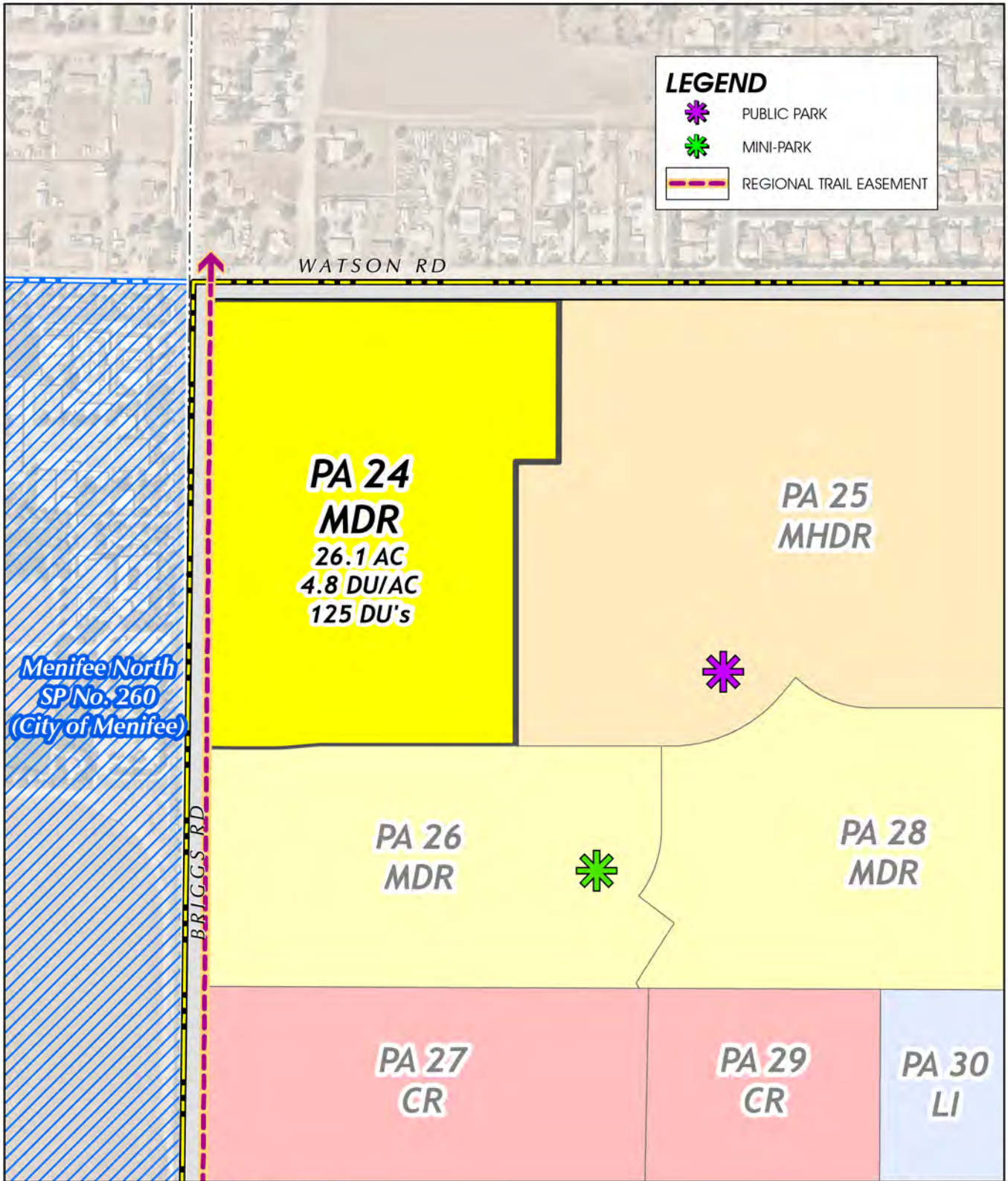
b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

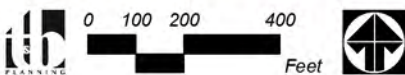
- 1) Access to Planning Area 24 shall be provided from Briggs Road.
- 2) Roadway landscape treatments, such as those depicted in Figures IV-17 and 18, shall be provided along Briggs Road, and Watson Road.
- 3) A drainage channel edge treatment shall be provided between the residential uses in Planning Area 24 and the adjacent drainage channel proposed along the northern edges of this planning area, as shown on Figure IV-9.
- 4) A special landscape treatment, as shown on Figure IV-8, shall be provided along Watson Road to limit views of this planning area from existing adjacent residential uses.
- 5) A regional trail easement is proposed along Briggs Road, as displayed on Figure IV-14.
- 6) A retaining wall, which results in an increase in the Solid Wall height of up to two (2) feet may be constructed along Planning Area 24's perimeter abutting Briggs Road.
- 7) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 8) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |



Source(s): ESRI, Near Map Aerial (2021), RCTLMA (2021)

FIGURE III-14



PLANNING AREA 24

III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

25. PLANNING AREA 25: MEDIUM-HIGH DENSITY RESIDENTIAL

a. Descriptive Summary

Planning Area 25, as depicted in Figure III-15, provides for development of 44.6 acres of Medium-High Density Residential uses with minimum lot sizes of 3,500 square feet. A maximum total of 253 dwelling units are planned at a target density of 5.7 du/ac. A 4.9-acre Public Park is located within Planning Area 25 and provides amenities that may include, but are not limited to, a sports field, basketball court, tot lots, shaded structures, and walking paths.

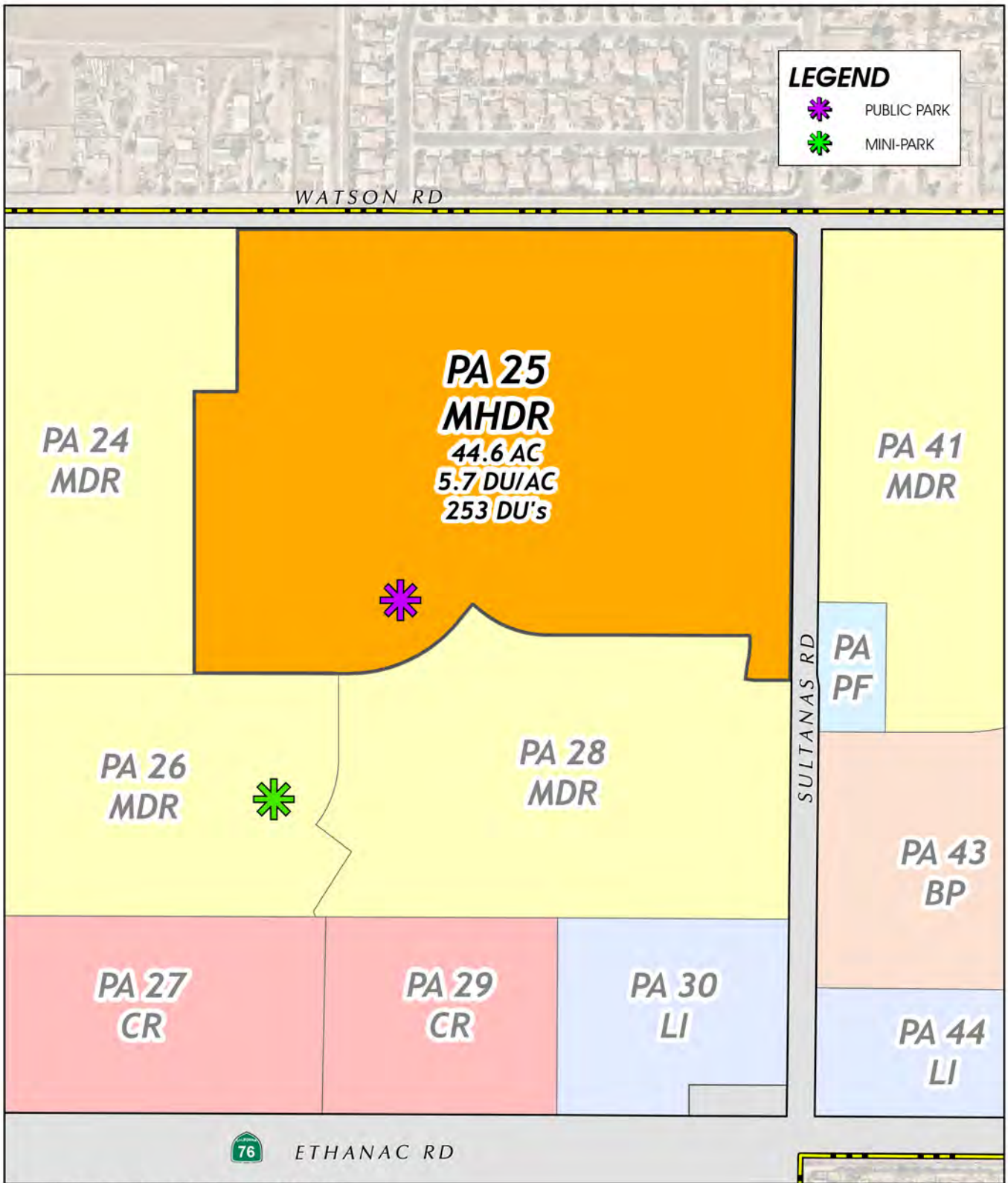
b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

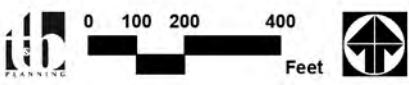
- 1) Access to Planning Area 25 shall be provided from Sultanas Road and Watson Road.
- 2) A special landscape treatment, as shown in Figure IV-8, shall be provided along Watson Road to limit views of this planning area from existing adjacent residential uses.
- 3) The Public Park within Planning Area 25 will be owned and maintained by the Valley-Wide Recreation and Park District.
- 4) Improvement plans for the Public Park within Planning Area 25 shall accompany the first development application for Planning Area 25.
- 5) The Public Park within Planning Area 25 shall be completed prior to the issuance of the 280th building permit in Planning Areas 24 and 25.
- 6) A drainage channel edge treatment, as depicted on Figure IV-9, shall be established between the residential uses in Planning Area 25 and the adjacent drainage channel proposed along the northern edge of the planning area.
- 7) A retaining wall, which results in an increase in the Solid Wall height of up to two (2) feet may be constructed along Planning Area 25's perimeter abutting Sultanas Road.
- 8) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 9) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |



Source(s): ESRI, Near Map Aerial (2021), RCTLMA (2021)

FIGURE III-15



PLANNING AREA 25
 III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

26. PLANNING AREA 26: MEDIUM DENSITY RESIDENTIAL

a. Descriptive Summary

Planning Area 26, as depicted in Figure III-16, provides for development of 19.6 acres of Medium Density Residential uses with minimum lot sizes of 6,000 square feet. A maximum total of 80 dwelling units are planned at a target density of 4.1 du/ac. A mini-park site is provided within Planning Area 26.

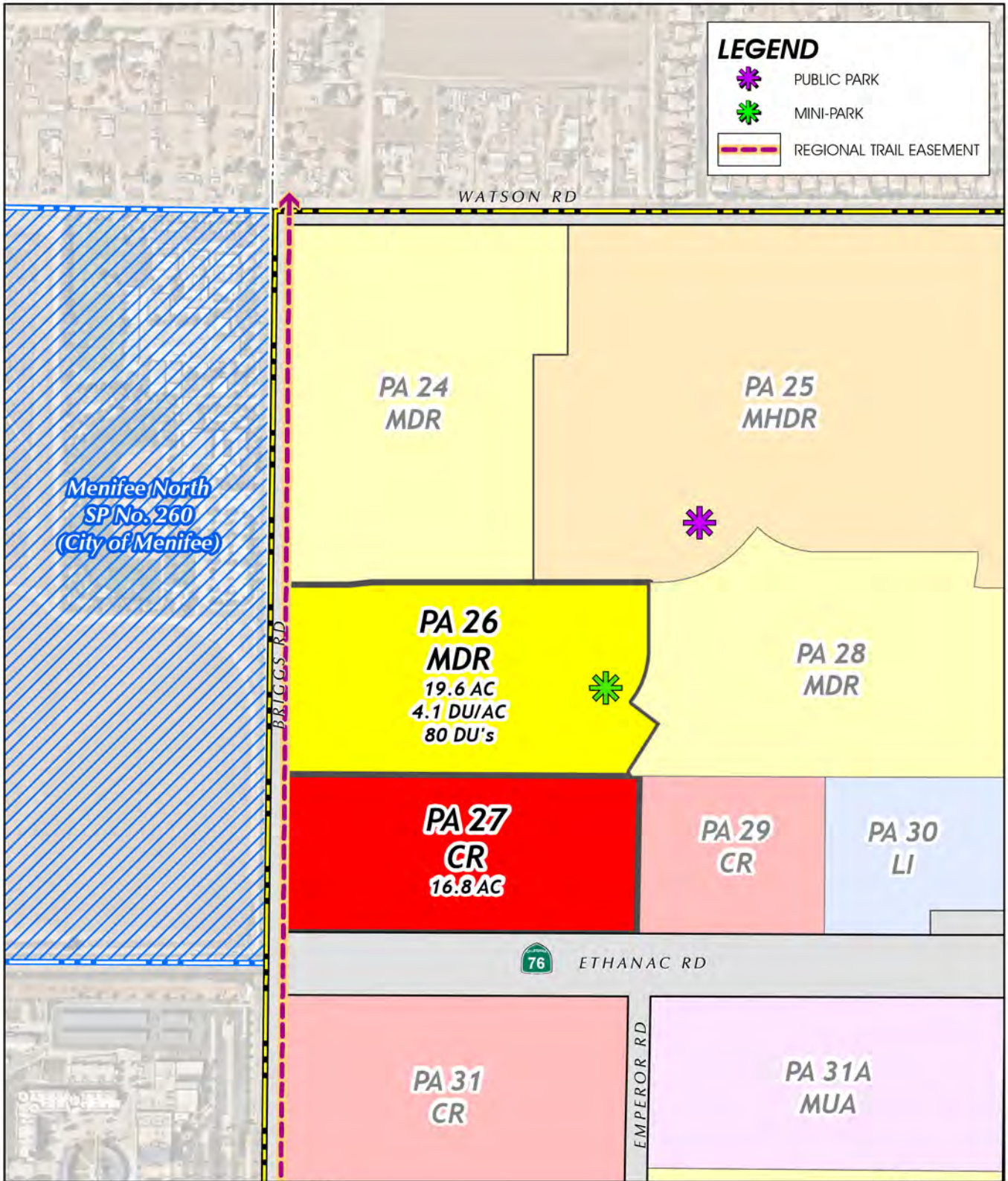
b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

- 1) Primary access to Planning Area 26 shall be provided from Briggs Road.
- 2) A regional trail easement, as shown on Figure IV-14, is proposed along Briggs Road at the planning area's western boundary.
- 3) Roadway landscape treatments, as illustrated in Figures IV-17, shall be provided along Briggs Road.
- 4) A drainage channel edge treatment, as depicted on Figure IV-10, shall be established between the residential uses in Planning Area 26 and the proposed drainage channel along Briggs Road.
- 5) A retaining wall, which results in an increase in the Solid Wall height of up to two (2) feet may be constructed along Planning Area 26's perimeter abutting Briggs Road.
- 6) Improvement plans for the mini-park site within Planning Area 26 shall accompany the first development application for Planning Area 26.
- 7) The mini-park within Planning Area 26 shall be completed prior to the issuance of the 60th building permit in Planning Area 26.
- 8) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 9) A special landscaped treatment, as depicted on Figure IV-12, shall be established between the residential uses in Planning Area 26 and the adjacent commercial retail uses in Planning Area 27.
- 10) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |



Source(s): ESRI, Near Map Aerial (2021), RCTLMA (2021)

FIGURE III-16

PLANNING AREA 26 & 27

III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

27. PLANNING AREA 27: COMMERCIAL RETAIL

a. Descriptive Summary

Planning Area 27, as depicted in Figure III-16, provides for development of 16.8 acres devoted to Commercial Retail uses.

b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

- 1) Primary access to Planning Area 27 shall be provided from Highway 74 (as approved by Cal- Trans) and Briggs Road.
- 2) A community entry/intersection statement, as depicted on Figure IV-2, shall be provided at the intersection of Highway 74 and Briggs Road.
- 3) A regional trail easement is proposed along the western boundary of Planning Area 27, as shown on Figure IV-14.
- 4) Roadway landscape treatments, as illustrated in Figures IV-16 and 17 are planned along Highway 74 and Briggs Road respectively.
- 5) A drainage channel edge treatment, as depicted on Figure IV-10, is proposed between the commercial uses in Planning Area 27 and the proposed drainage channel along Briggs Road.
- 6) In conjunction with the first implementing plot plan submitted for this planning area, a conceptual site plan for the entire planning area shall be provided to the Riverside County Planning Department for informational purposes. This site plan shall show how the proposed plot plan integrates into the remainder of the planning area, to ensure proper design and layout of the parking and street access.
- 7) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 8) A special landscaped treatment, as depicted on Figure IV-12, shall be established between the commercial uses in Planning Area 27 and the adjacent residential uses in Planning Area 26.
- 9) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

III.A.1: Specific Land Use Plan	III.A.6: Grading Plan
III.A.2: Circulation Plan	III.A.7: Landscaping Plan
III.A.3: Drainage Plan	III.A.8: Public Facilities Phasing Plan
III.A.4: Water and Sewer Plans	III.A.9: Phasing Plan
III.A.5: Open Space and Recreation Plan	

28. PLANNING AREA 28: MEDIUM DENSITY RESIDENTIAL

a. Descriptive Summary

Planning Area 28, as depicted on Figure III-17, provides for the development of 21.8 acres of Medium Density Residential uses with minimum lot sizes of 6,000 square feet. A maximum total of 93 dwelling units are planned at a target density of 4.2 du/ac.

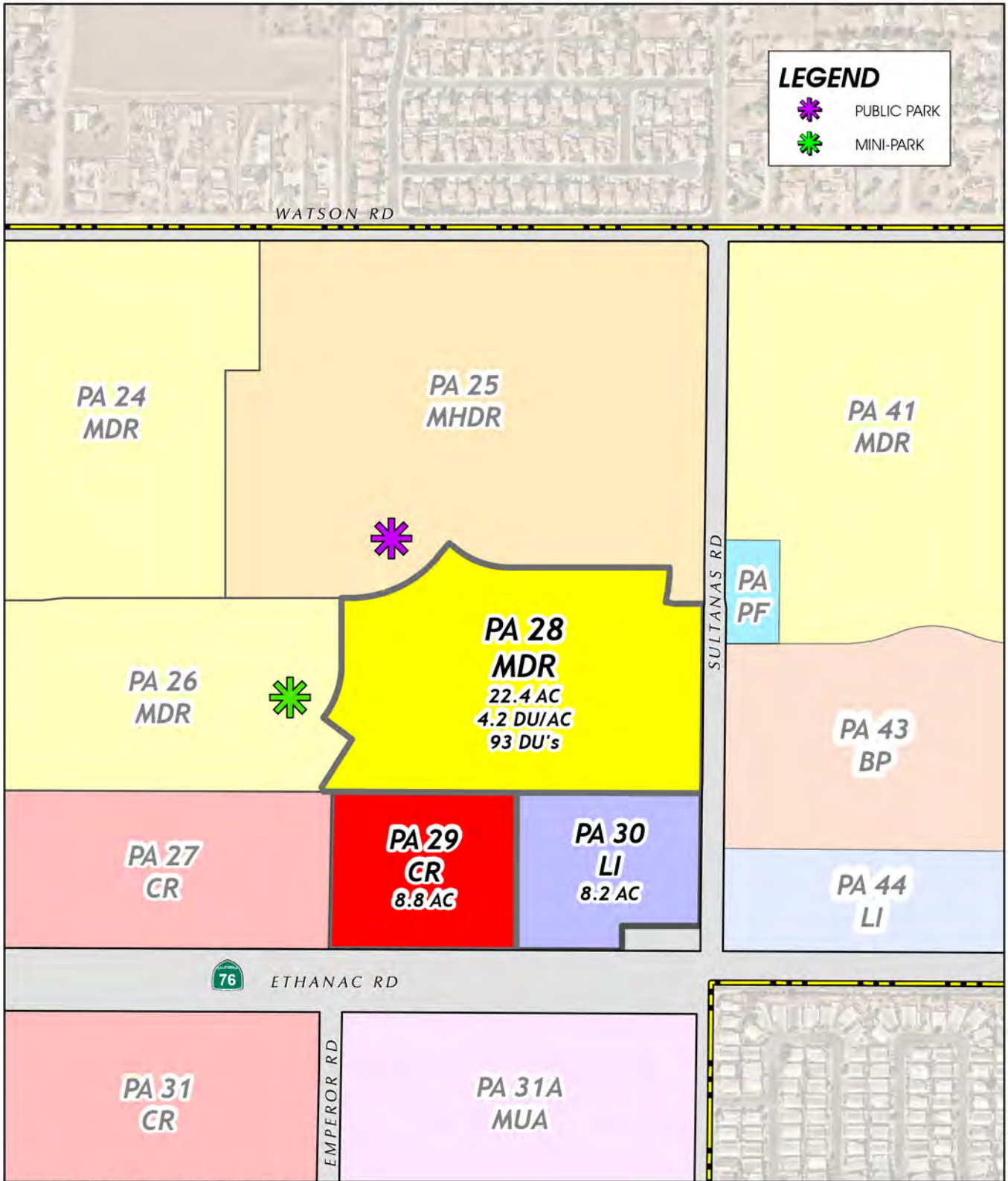
b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

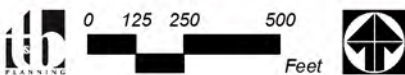
- 1) Primary access to Planning Area 28 shall be provided from Sultanas Road.
- 2) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 3) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:
- 4) A special landscaped treatment, as depicted on Figure IV-12, shall be established between the residential uses in Planning Area 28 and the adjacent non-residential uses in Planning Areas 29 and 30.
- 5) A retaining wall, which results in an increase in the Solid Wall height of up to two (2) feet may be constructed along Planning Area 28's perimeter abutting Sultanas Road.
- 6) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |



Source(s): ESRI, Near Map Aerial (2021), RCTLMA (2021)

FIGURE III-17



PLANNING AREA 28, 29, & 30

III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

29. PLANNING AREA 29: COMMERCIAL RETAIL

a. Descriptive Summary

Planning Area 29, as depicted on Figure III-17, provides for the development of 8.8 acres devoted to Commercial Retail uses.

b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

- 1) Primary access to Planning Area 29 shall be provided from Highway 74 (as approved by Cal- Trans).
- 2) Roadway landscape treatments, as illustrated in Figures IV-16 shall be established along Highway 74.
- 3) In conjunction with the first implementing plot plan submitted for this planning area, a conceptual site plan for the entire planning area shall be provided to the Riverside County Planning Department for informational purposes. This site plan shall show how the proposed plot plan integrates into the remainder of the planning area, to ensure proper design and layout of the parking and street access.
- 4) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 5) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:
- 6) A special landscaped treatment, as depicted on Figure IV-12, shall be established between the commercial retail uses in Planning Area 29 and the adjacent residential uses in Planning Area 28.
- 7) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

III.A.1: Specific Land Use Plan
III.A.2: Circulation Plan
III.A.3: Drainage Plan
III.A.4: Water and Sewer Plans
III.A.5: Open Space and Recreation Plan

III.A.6: Grading Plan
III.A.7: Landscaping Plan
III.A.8: Public Facilities Phasing Plan
III.A.9: Phasing Plan

30. PLANNING AREA 30: LIGHT INDUSTRIAL

a. Descriptive Summary

Planning Area 30, as depicted in Figure III-17, provides for development of 8.2 acres devoted to Light Industrial uses.

b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

- 1) Primary access to Planning Area 30 shall be provided from Highway 74 (as approved by CalTrans) and Sultanas Road.
- 2) A project entry/intersection statement, as depicted on Figure IV-3, shall be provided at the intersection of Highway 74 and Sultanas Road.
- 3) Roadway landscape treatments, as illustrated in Figures IV-16 and 18, are planned along Highway 74 and Sultanas Road, respectively.
- 4) In conjunction with the first implementing plot plan submitted for this planning area, a conceptual site plan for the entire planning area shall be provided to the Riverside County Planning Department for informational purposes. This site plan shall show how the proposed plot plan integrates into the remainder of the planning area, to ensure proper design and layout of the parking and street access.
- 5) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 6) A special landscaped treatment, as depicted on Figure IV-12, shall be established between the light industrial uses in Planning Area 30 and the adjacent residential uses in Planning Area 28.
- 7) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

III.A.1: Specific Land Use Plan	III.A.6: Grading Plan
III.A.2: Circulation Plan	III.A.7: Landscaping Plan
III.A.3: Drainage Plan	III.A.8: Public Facilities Phasing Plan
III.A.4: Water and Sewer Plans	III.A.9: Phasing Plan
III.A.5: Open Space and Recreation Plan	

31. PLANNING AREA 31: COMMERCIAL RETAIL

a. Descriptive Summary

Planning Area 31, as depicted in Figure III-18, provides for development of 32.6 acres devoted to Commercial Retail uses.

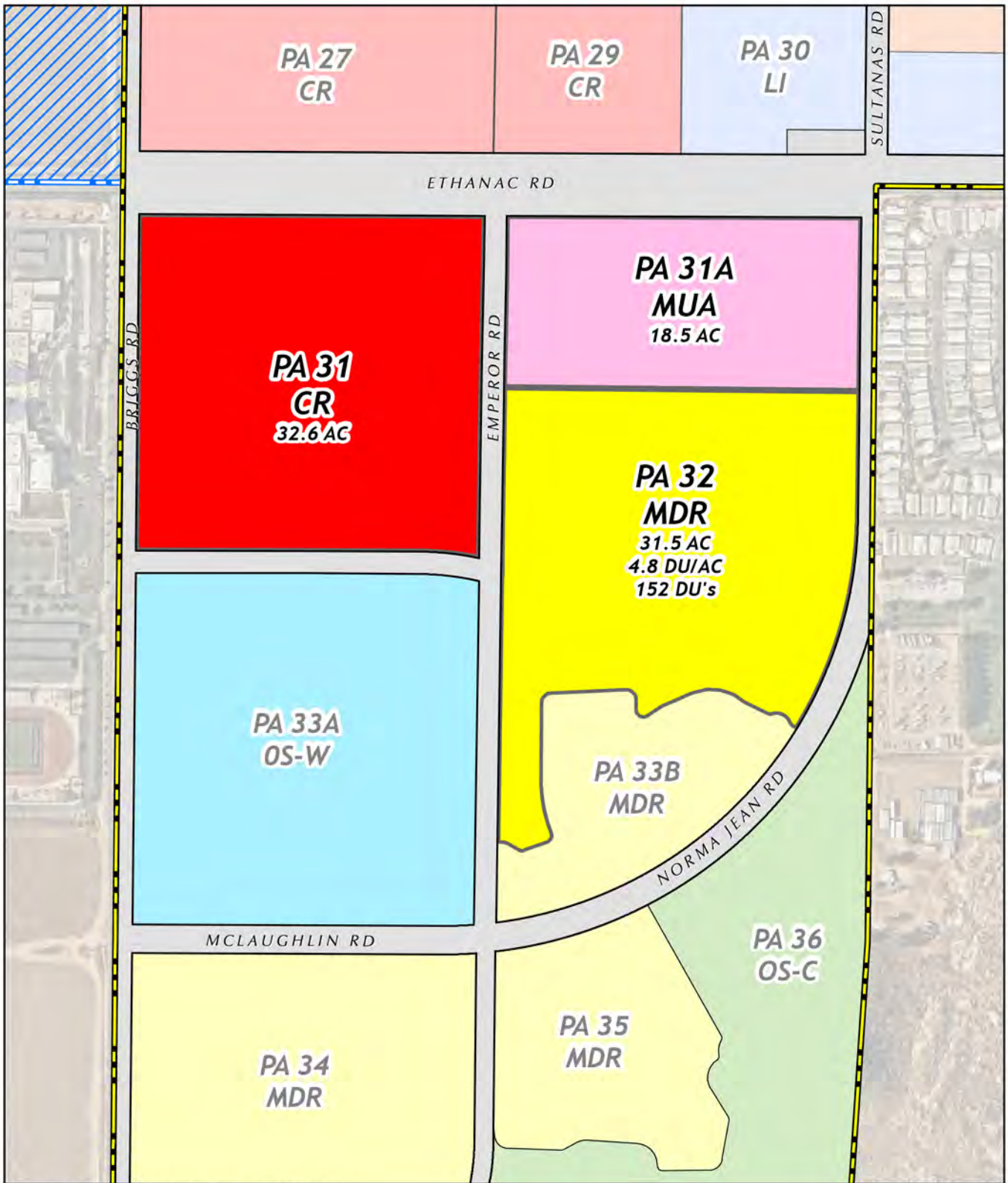
b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

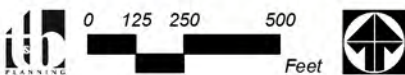
- 1) Primary access to Planning Area 31 shall be provided from Highway 74 (right-turn in and out only as approved by CalTrans), Emperor Road, Street "B," and Briggs Road.
- 2) A community entry/intersection statement, as depicted on Figure IV-2, shall be constructed at the intersection of Highway 74 and Briggs Road.
- 3) A regional trail easement, as depicted on Figure IV-14, is proposed at the western boundary of Planning Area 31, along Briggs Road.
- 4) Roadway landscape treatments, as illustrated in Figures IV-16, 17, and 18, are planned along Highway 74, Briggs Road, Street "B," and Emperor Road, respectively.
- 5) A drainage channel edge treatment, as depicted on Figure IV-10, is proposed between the commercial retail uses in Planning Area 31 and the proposed drainage channel along Briggs Road.
- 6) In conjunction with the first implementing plot plan submitted for this planning area, a conceptual site plan for the entire planning area shall be provided to the Riverside County Planning Department for informational purposes. This site plan shall show how the proposed plot plan integrates into the remainder of the planning area, to ensure proper design and layout of the parking and street access.
- 7) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 8) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |



Source(s): ESRI, Near Map Aerial (2021), RCTLMA (2021)

FIGURE III-18



PLANNING AREA 31, 31A, & 32

III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

31A. PLANNING AREA 31A: MIXED USE PLANNING AREA

a. Descriptive Summary

Planning Area 31A, as depicted on Figure III-18, provides for the development of 18.5 acres of Mixed Use. This planning area could develop with a mix of limited commercial and high density residential land uses. The maximum density for residential use is 16.0 du/ac. The maximum DU shall not exceed 300 subject to County approval of plot plan layouts and density.

b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

- 1) Access to Planning Area 31A shall be provided from Emperor Road and Highway 74 (as approved by CalTrans). Access shall be prohibited from Norma Jean Road.
- 2) A project entry/intersection statement, as depicted on Figure IV-3, shall be provided at the intersection of Highway 74 and Sultanas Road.
- 3) Roadway landscape treatments, as illustrated in Figure IV-16 shall be established along Highway 74.
- 4) To limit views of this planning area from existing adjacent residential uses to the east, a special roadway landscape treatment shall be provided on Sultanas Road, as shown in Figure 19.
- 5) A landscape transition area shall be provided between the commercial uses in Planning Area 31A and the adjacent residential uses in Planning Area 32, as shown in Figure IV-12.
- 6) In conjunction with the first implementing plot plan submitted for this planning area, a conceptual site plan for the entire planning area shall be provided to the Riverside County Planning Department for informational purposes. This site plan shall show how the proposed plot plan integrates into the remainder of the planning area, to ensure proper design and layout of the parking and street access.
- 7) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 8) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |

32. PLANNING AREA 32: MEDIUM DENSITY RESIDENTIAL

a. Descriptive Summary

Planning Area 32, as depicted on Figure III-18, provides for the development of 31.5 acres of medium density residential uses with minimum lot sizes of 5,000 square feet. A maximum total of 152 dwelling units are planned at a target density of 4.8 du/ac.

b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

- 1) Access to Planning Area 32 shall be provided from Norma Jean/Sultanas Road and Emperor Road.
- 2) A landscape transition area, as shown on Figure IV-12, shall be established between the residential uses in Planning Area 32 and the adjacent mixed use development in Planning Area 31A.
- 3) To limit views of this planning area from existing adjacent residential uses to the east, a special roadway landscape treatment shall be provided on Norma Jean/Sultanas Road, as shown in Figure 18A.
- 4) A mini-park shall be developed within Planning Area 32, comprising approximately 4 acres.
- 5) A 6-foot tall combo wall composed of a low splitface block wall with tubular steel view fence shall be developed along the rear property lines of residential lots that abut the Mini-Park.
- 6) Where the project site boundary abuts Paradise Palm Drive, access to the adjacent golf course will be re-established at no cost to Highland Palm residents as right-of-way improvements in this area proceed.
- 7) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 8) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |

33A. PLANNING AREA 33A: OPEN SPACE-WATER

a. Descriptive Summary

Planning Area 33A, as depicted in Figure III-19, provides for development of a 33.8 acre drainage basin, with a designated use of Open Space-Water. This drainage basin is a significant part of the master drainage plan for the Specific Plan.

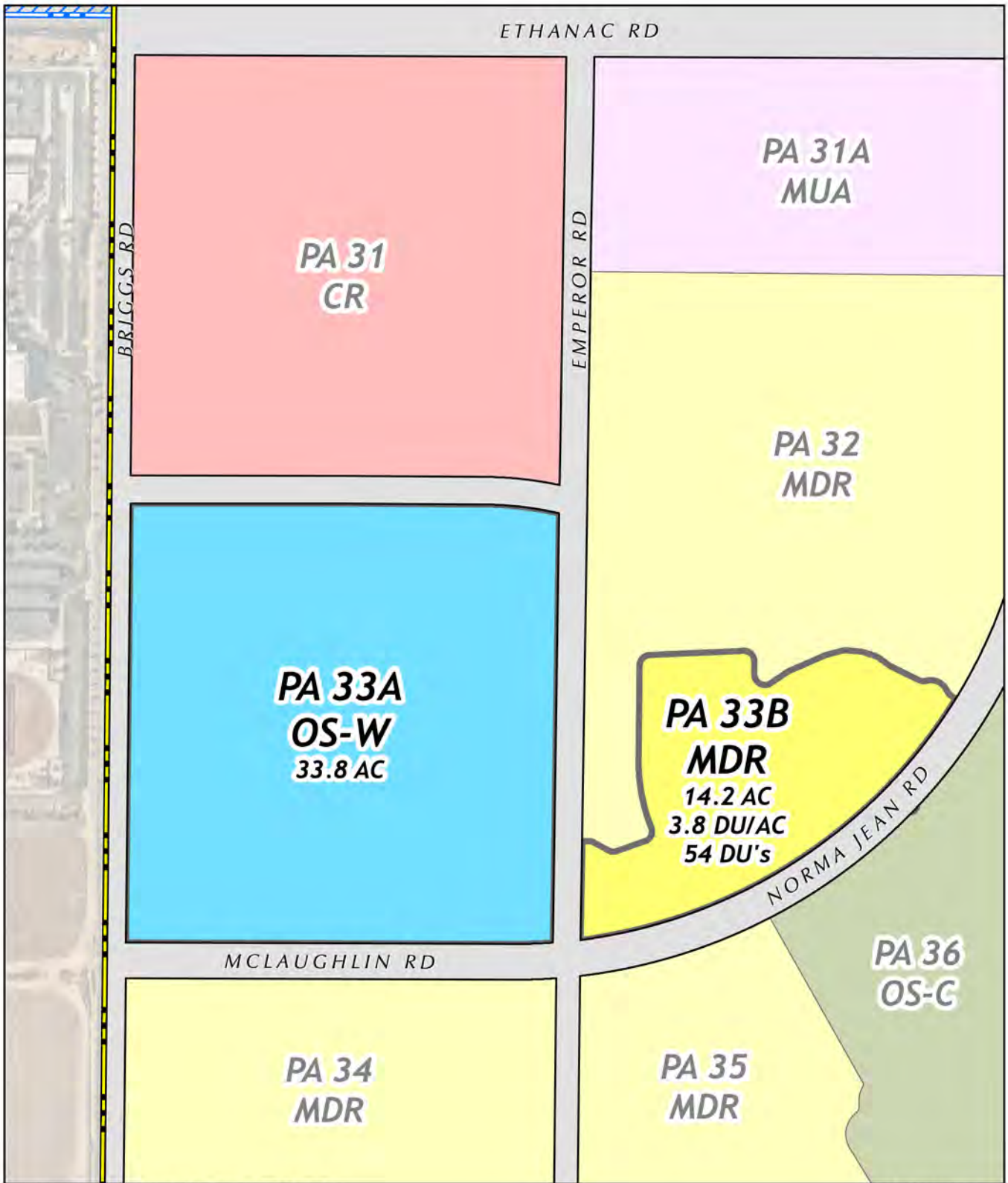
b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

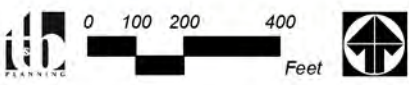
- 1) Access to Planning Area 33A shall be provided from Street "B", Briggs Road and McLaughlin Road.
- 2) A regional trail easement, as depicted on Figure IV-14, is proposed to border Planning Area 33A on the west, along Briggs Road.
- 3) Roadway landscape treatments, as depicted in Figure IV-17 and Figure IV-18, shall be provided along Briggs Road and Street "B", respectively.
- 4) A drainage basin edge treatment shall be established between Briggs Road and McLaughlin Road and the basin, as shown in Figure IV-9.
- 5) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |



Source(s): ESRI, Near Map Aerial (2021), RCTLMA (2021)

FIGURE III-19



PLANNING AREA 33A & 33B

III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

33B. PLANNING AREA 33B: MEDIUM DENSITY RESIDENTIAL

a. Descriptive Summary

Planning Area 33B, as depicted in Figure III-19, provides for development of 14.2 acres of medium density residential uses with minimum lot sizes of 5,000 square feet. A maximum total of 54 dwelling units are planned at a target density of 3.8 du/ac.

b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

- 1) Access to Planning Area 33B shall be provided from Norma Jean Road.
- 2) A 6-foot tall combo wall composed of a low splitface block wall with tubular steel view fence shall be developed along the rear property lines of residential lots that abut the Mini-Park located in Planning Area 32.
- 3) Where the project site boundary abuts Paradise Palm Drive, access to the adjacent golf course will be re-established at no cost to Highland Palm residents as right-of-way improvements in this area proceed.
- 4) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 5) Please refer to Section III.A for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |

33. PLANNING AREA 34: MEDIUM DENSITY RESIDENTIAL

a. Descriptive Summary

Planning Area 34, as depicted in Figure III-20, provides for development of 80.3 acres of medium density residential uses with minimum lot sizes of 5,000 square feet. A maximum total of 339 dwelling units are planned at a target density of 4.2 du/ac. The number of dwelling units is restricted to accommodate several neighborhoods of differing lot sizes. Lot sizes shall not exceed 10,000 square feet. The maximum density may be increased to 6.0 du/ac with a 5,000 square foot minimum lot size if this planning area is designed for and restricted to senior citizen housing or if this planning area is designed as a mobile home park or subdivision.

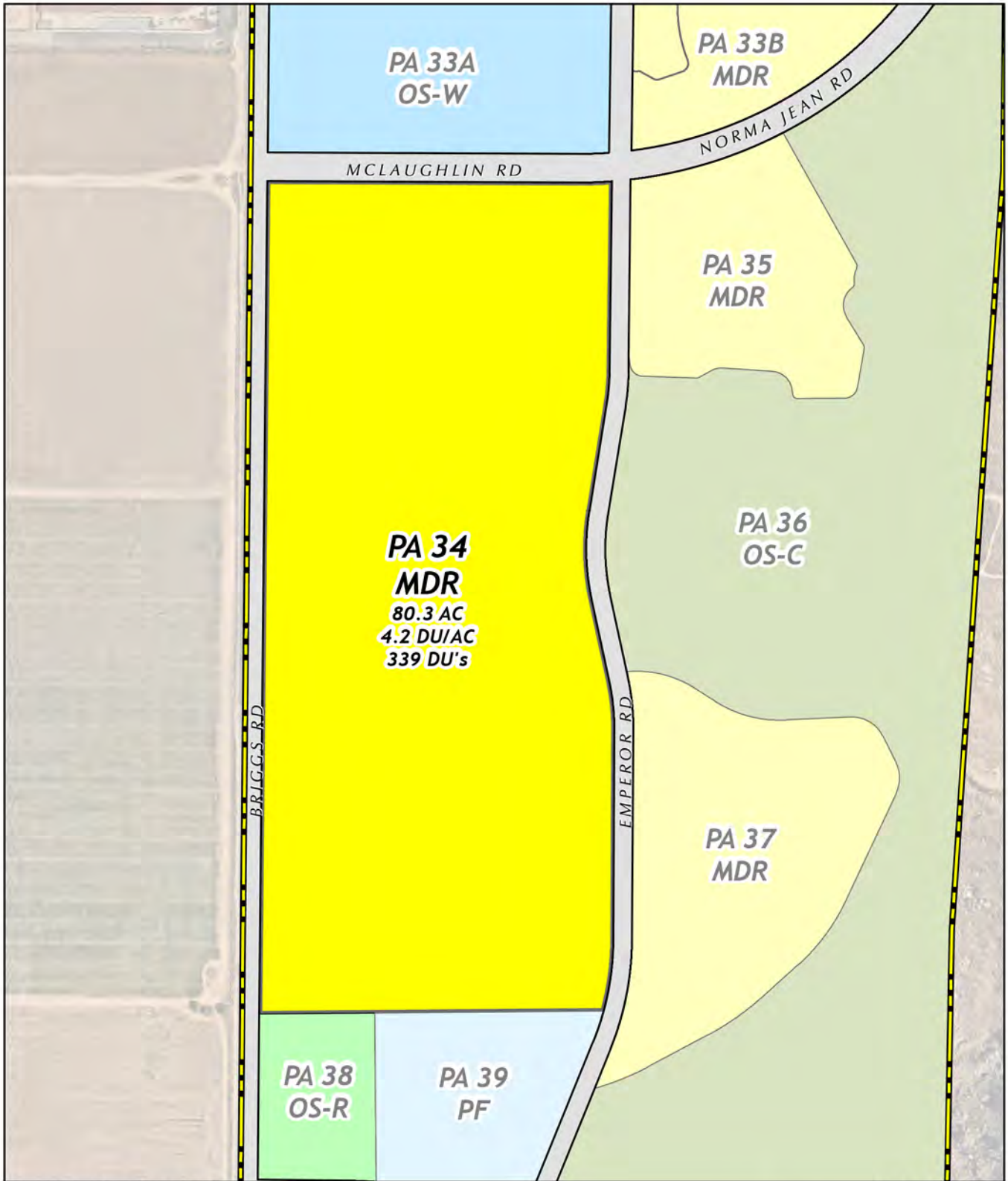
b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

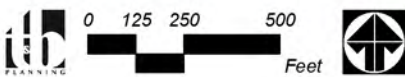
- 1) Access to Planning Area 34 shall be provided from Briggs Road, Emperor Road and McLaughlin Road.
- 2) A project entry/intersection statement, as shown on Figure IV-3, shall be developed at the intersection of McLaughlin Road and Briggs Road.
- 3) A roadway landscape treatment, as depicted in Figure IV-17, shall be provided along Briggs Road.
- 4) Special landscape transition areas shall be established between the residential uses in Planning Area 34 and the adjacent park and school uses in Planning Areas 38 and 39, as shown in Figure IV-11.
- 5) A regional trail easement, as depicted on Figure IV-14, is proposed to border Planning Area 34 to the north along McLaughlin Road.
- 6) A more detailed geologic investigation shall be performed for development adjacent to the steep hillside (Double Butte margin) area relative to slope stability, rock fall and debris flow.
- 7) A drainage channel edge treatment, as depicted in Figure IV-9, shall be established between the residential uses in Planning Area 34 and the proposed drainage channel along Briggs Road.
- 8) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 9) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |



Source(s): ESRI, Near Map Aerial (2021), RCTLMA (2021)

FIGURE III-20



PLANNING AREA 34

III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

34. PLANNING AREA 35: MEDIUM DENSITY RESIDENTIAL

a. Descriptive Summary

Planning Area 35, as depicted on Figure III-21, provides for the development of 13.2 acres of medium density residential uses with minimum lot sizes of 6,000 square feet. A maximum total of 43 dwelling units are planned at a target density of 3.3 du/ac.

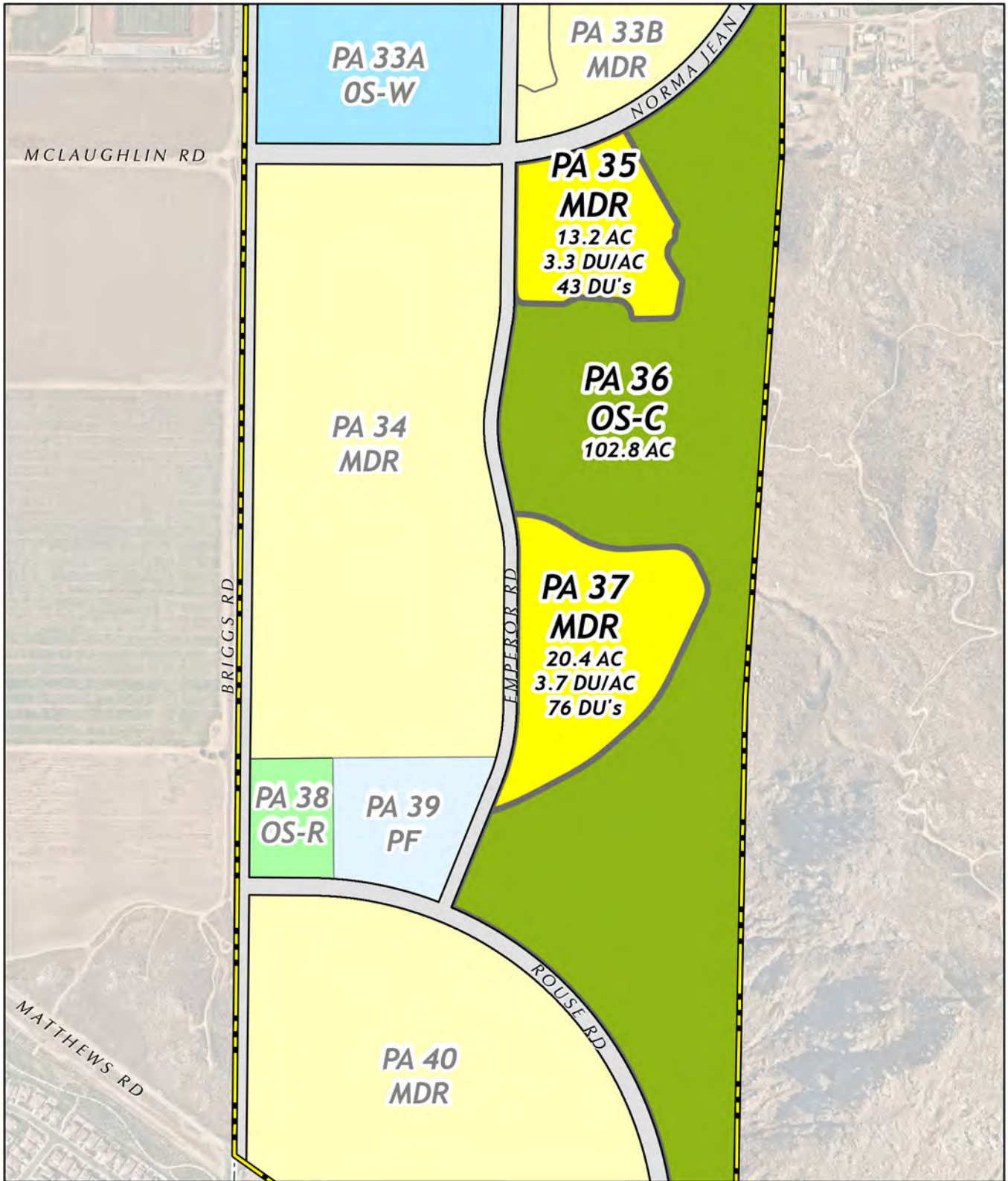
b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

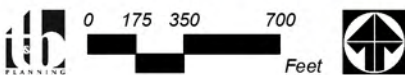
- 1) Access to Planning Area 35 shall be provided from McLaughlin Road and Emperor Road.
- 2) A regional trail easement, as depicted on Figure IV-14, is proposed along the western edge of Planning Area 35, adjacent to Emperor Road.
- 3) A more detailed geologic investigation shall be performed for development adjacent to the steep hillside (Double Butte margin) area relative to slope stability, rock fall and debris flow.
- 4) A 0.4-acre mini-park shall be developed within Planning Area 35.
- 5) Where the project site boundary abuts Paradise Palm, access to the adjacent golf course will be re-established at no cost to Highland Palm residents as right-of-way improvements in this area proceed.
- 6) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 7) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |



Source(s): ESRI, Near Map Aerial (2021), RCTLMA (2021)

FIGURE III-21



PLANNING AREA 35, 36, & 37

III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

35. PLANNING AREA 36: OPEN SPACE-CONSERVATION

a. Descriptive Summary

Planning Area 36, as depicted on Figure III-21, provides for preservation of 102.8 acres for natural open space.

b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

- 1) Access to Planning Area 36 shall be provided from Emperor Road and Rouse Road.
- 2) A regional trail easement, as depicted on Figures IV-13 and 14, is proposed to follow the western-most boundary of Planning Area 36, adjacent to Rouse Road, Emperor Road, and Planning Area 37.
- 3) A roadway landscape treatment, as depicted in Figure IV-18, shall be provided along Rouse Road.
- 4) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 5) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |

36. PLANNING AREA 37: MEDIUM DENSITY RESIDENTIAL

a. Descriptive Summary

Planning Area 37, as depicted on Figure III-21, provides for the development of 20.4 acres of medium density residential uses with minimum lot sizes of 6,000 square feet. A maximum total of 76 dwelling units are planned at a target density of 3.7 du/ac.

b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

- 1) Access to Planning Area 37 shall be provided from Emperor Road.
- 2) A regional trail easement, as depicted on Figure IV-13, is proposed along the boundary of Planning Area 37, adjacent to Planning Area 36 (the natural open space area).
- 3) A more detailed geologic investigation shall be performed for development adjacent to steep hillside (Double Butte margin) area relative to slope stability, rock fall and debris flow.
- 4) A 0.2-acre mini-park shall be developed within Planning Area 37.
- 5) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 6) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |

37. PLANNING AREA 38: OPEN SPACE-RECREATION

a. Descriptive Summary

Planning Area 38, as depicted in Figure III-22, provides for the development of 5.5 acres as a Community Park. Planning Area 38 will be landscaped and, at a minimum, will include such uses as picnic areas, tot lots, restrooms and parking facilities. In addition, the park may include two or more of the following: exercise course, playfield, basketball (half-court), sand volleyball court, soccer field, baseball field, group barbecue or shade arbor. A conceptual site plan is provided in the Design Guidelines, Section IV (Figure IV-15).

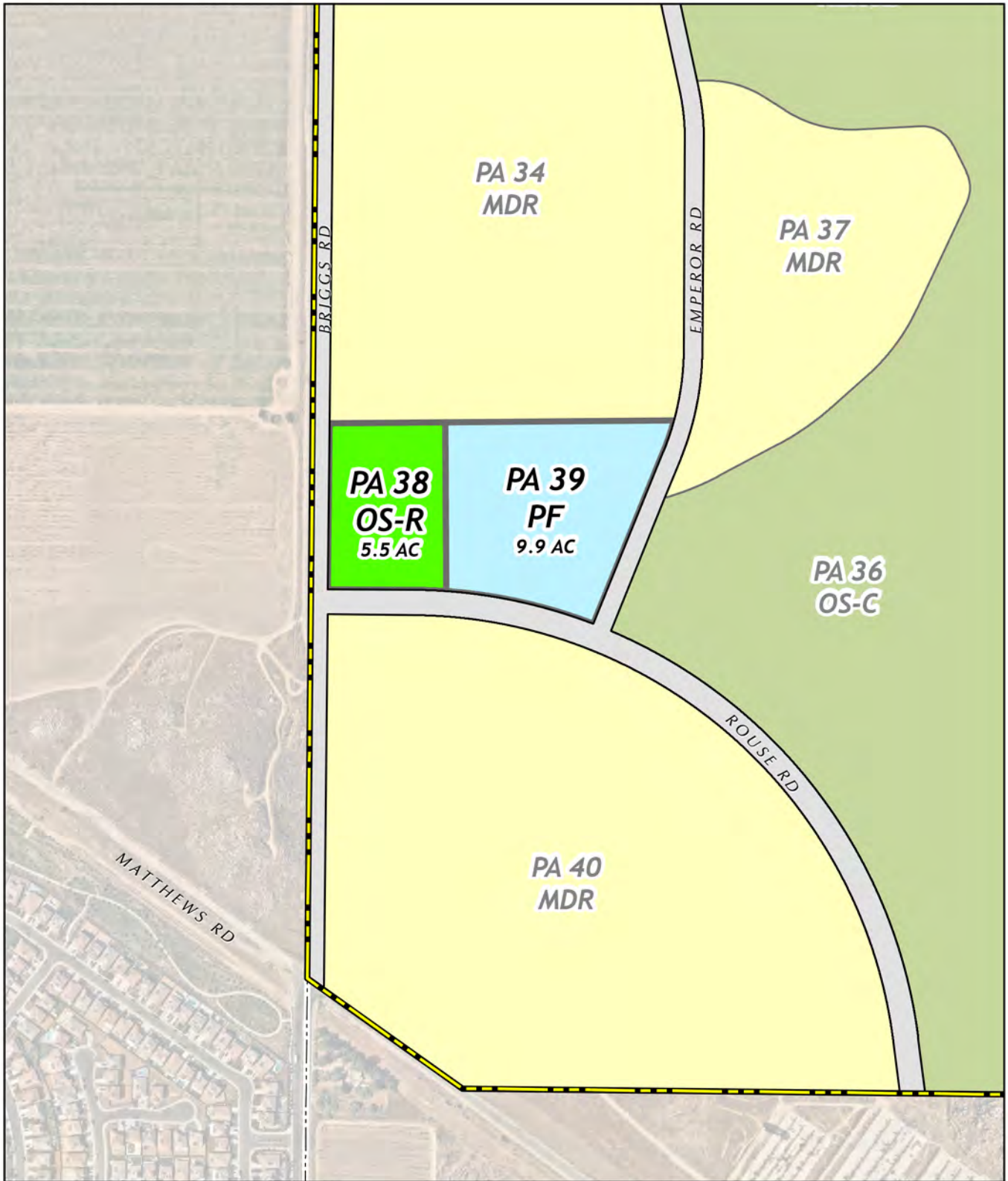
b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

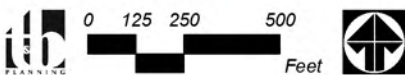
- 1) Access to Planning Area 38 shall be provided from Briggs Road and Rouse Road.
- 2) A special landscape transition area shall be established between the park uses in Planning Area 38 and the adjacent residential uses in Planning Area 34, as shown on Figure IV-11.
- 3) A special landscape treatment, as depicted on Figure IV-15, shall be established between the park uses in Planning Area 38 and the adjacent school in Planning Area 39.
- 4) Roadway landscape treatments, such as those depicted in Figures IV-17 and 18, shall be provided along Briggs Road and Rouse Road, respectively.
- 5) A drainage channel edge treatment, as depicted in Figure IV-9, is proposed between the park uses in Planning Area 38 and the drainage channel proposed along Briggs Road.
- 6) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 7) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |



Source(s): ESRI, Near Map Aerial (2021), RCTLMA (2021)

FIGURE III-22



PLANNING AREA 38 & 39

III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

38. PLANNING AREA 39: PUBLIC FACILITY

a. Descriptive Summary

Planning Area 39, as depicted on Figure III-22, provides for development of 9.9 acres as an Elementary School. If at some future point in time the school district should decline to purchase this site for development of an elementary school, then the project proponent reserves the right to develop this site with Medium-High Density Residential use. A maximum total of 50 dwelling units would be allowed at a target density of 5.0 du/ac. This may be allowable by transferring an equal number of excess units available from previously developed planning area(s) which do not accomplish the maximum number permitted for those planning areas. In any case, the total number of permitted units for the Specific Plan area shall not be exceeded.

b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

- 1) Access to Planning Area 39 shall be provided from Rouse Road and Emperor Road.
- 2) A roadway landscape treatment, as depicted in Figure IV-18, shall be provided along Rouse Road.
- 3) A special landscape treatment shall be established between the school uses in Planning Area 39 and the adjacent park uses in Planning Area 38, as shown on Figure IV-15.
- 4) A landscape transition area, as shown in Figure IV-11, shall be provided between the school uses in Planning Area 39 and the adjacent residential uses in Planning Area 34.
- 5) A more detailed geologic investigation shall be performed for development adjacent to the steep hillside (Double Butte margin) area relative to slope stability, rock fall and debris flow.
- 6) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 7) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |

39. PLANNING AREA 40: MEDIUM DENSITY RESIDENTIAL

a. Descriptive Summary

Planning Area 40, as depicted on Figure III-23, provides for development of 60.2 acres of medium density residential uses with minimum lot sizes of 6,000 square feet. A maximum total of 272 dwelling units are planned at a target density of 4.5 du/ac.

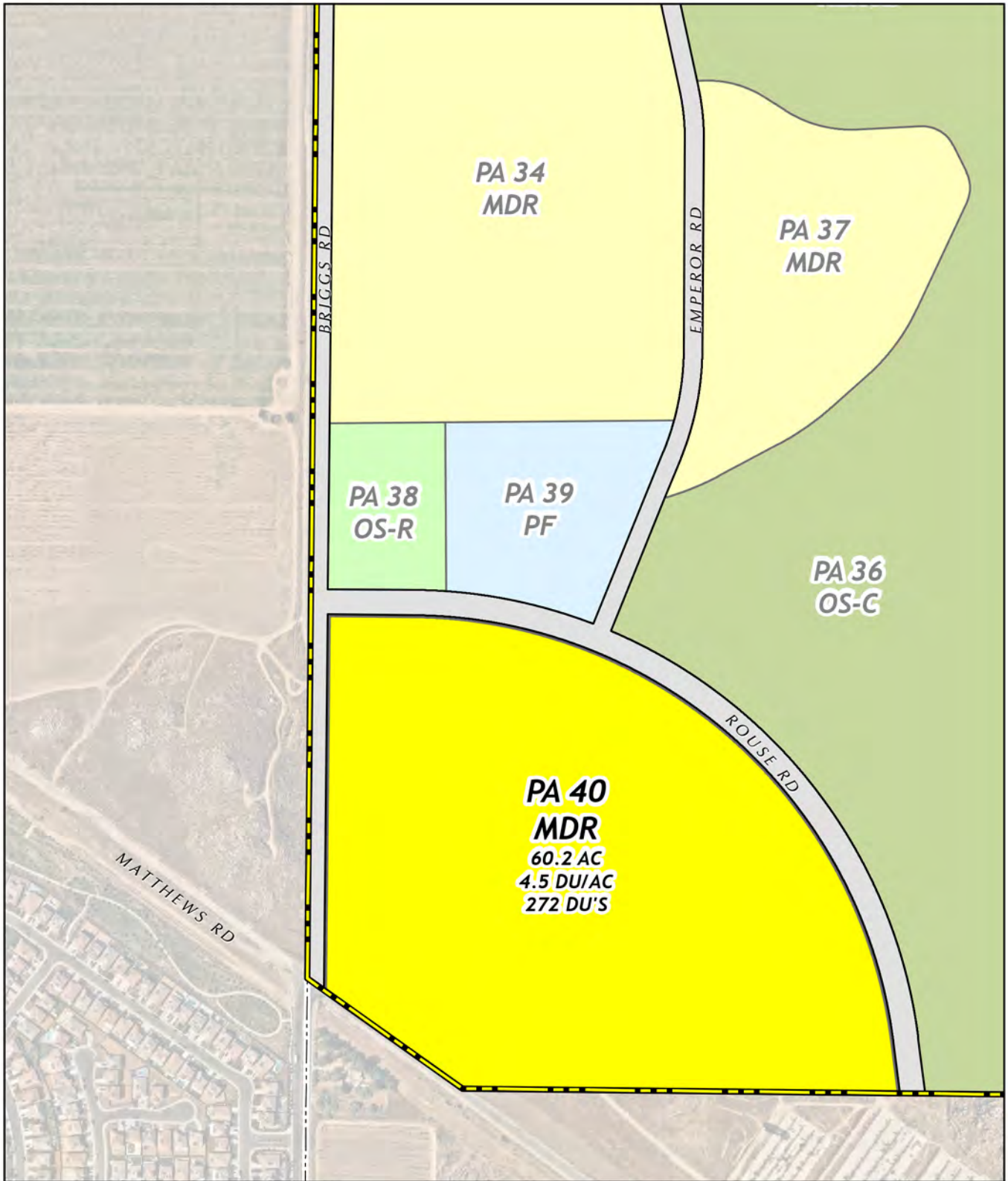
b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

- 1) Access to Planning Area 40 shall be provided from Rouse Road and Briggs Road.
- 2) Roadway landscape treatments, such as those depicted in Figures IV-17 and 18, shall be provided along Briggs Road and Rouse Road, respectively.
- 3) A special landscape treatment, as shown in Figure IV-7, shall be established between the residential uses in Planning Area 40 and the existing adjacent residential uses and the existing Metrolink line.
- 4) A drainage channel edge treatment, as shown in Figure IV-9, shall be established between the residential uses in Planning Area 40 and the proposed drainage channel along Briggs Road.
- 5) A more detailed geologic investigation shall be performed for development adjacent to the steep hillside (Double Butte margin) area relative to slope stability, rock fall and debris flow.
- 6) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 7) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |



Source(s): ESRI, Near Map Aerial (2021), RCTLMA (2021)

FIGURE III-23



PLANNING AREA 40

III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

40. PLANNING AREA 41: MEDIUM DENSITY RESIDENTIAL

a. Descriptive Summary

Planning Area 41, as depicted on Figure III-24, provides for development of 39.8 acres of medium density residential uses with minimum lot sizes of 7,200 square feet. A maximum total of 120 dwelling units are planned at a target density of 3.0 du/ac.

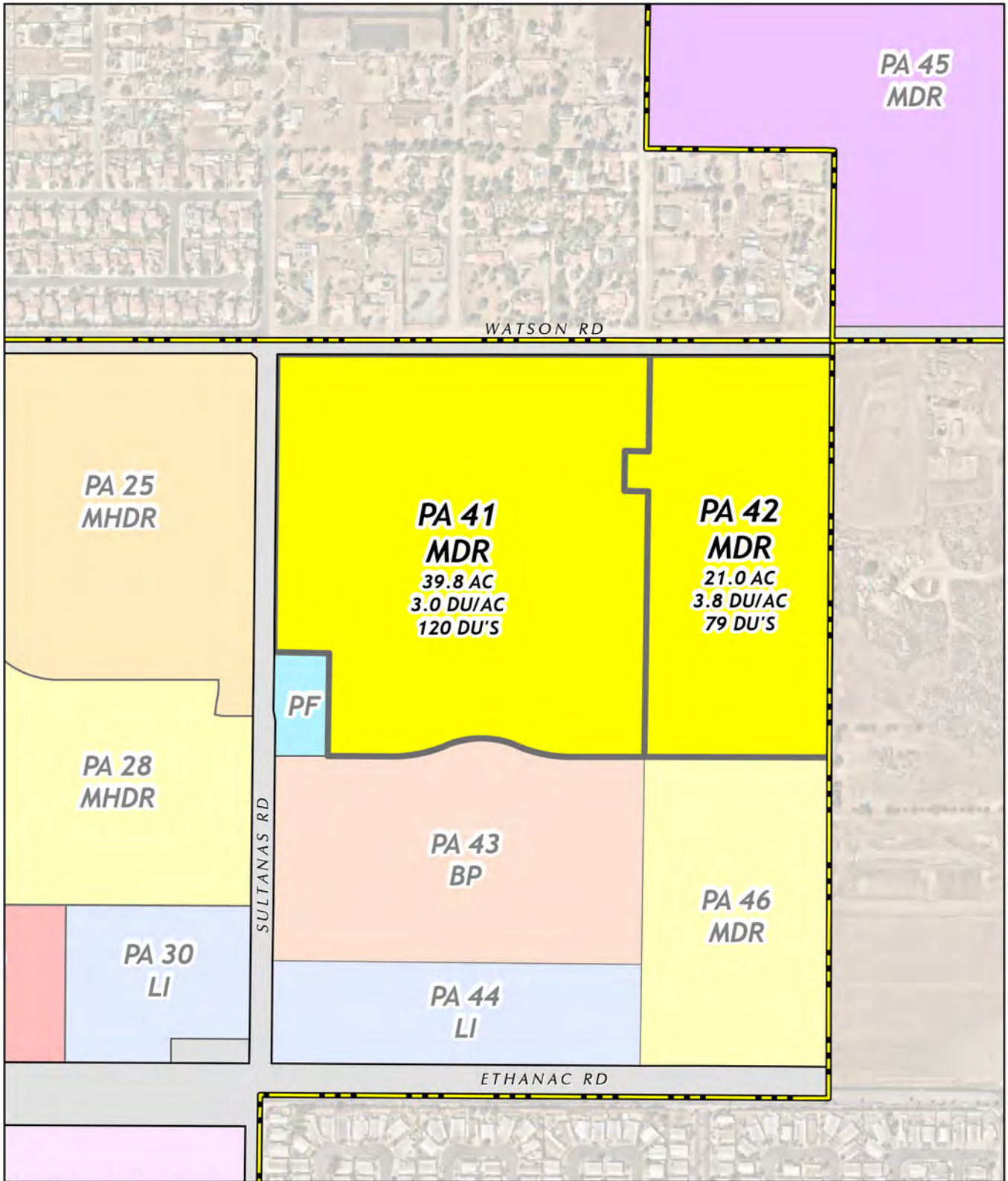
b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

- 1) Access to Planning Area 41 shall be provided from Sultanas Road and Watson Road.
- 2) A minor intersection statement shall be developed at the intersection of Sultanas Road and the southwest corner of the fire station parcel, as shown on Figure IV-4.
- 3) A drainage channel edge treatment, as depicted on Figure IV-9, shall be established between the residential uses in Planning Area 41 and the drainage channel proposed adjacent to Watson Road.
- 4) Roadway landscape treatments, as depicted in Figure IV-18, shall be provided along Watson Road .
- 5) A special landscape treatment area shall be established between the residential uses in Planning Area 41 and the adjacent business park uses in Planning Area 43, as shown on Figure IV-12.
- 6) A special landscape treatment, as shown on Figure IV-8, shall be provided along Watson Road to limit views of this planning area from existing adjacent residential uses.
- 7) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 8) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |



Source(s): ESRI, Near Map Aerial (2021), RCTLMA (2021)

FIGURE III-24



PLANNING AREA 41 & 42

III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

41. PLANNING AREA 42: MEDIUM DENSITY RESIDENTIAL

a. Descriptive Summary

Planning Area 42, as depicted on Figure III-24, provides for the development of 21.0 acres of medium density residential uses with minimum lot sizes of 7,200 square feet. A maximum total of 79 dwelling units are planned at a target density of 3.8 du/ac.

b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

- 1) Access to Planning Area 42 shall be provided from Watson Road and/or a local road from the south.
- 2) A drainage channel edge treatment, as depicted on Figure IV-9, shall be established between the residential uses in Planning Area 42 and the proposed drainage channel adjacent to Watson Road.
- 3) A special landscape treatment, as shown on Figure IV-8, shall be provided along Watson Road to limit views of this planning area from existing adjacent residential uses.
- 4) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 5) Roadway landscape treatments, as depicted in Figure IV-18, shall be provided along Watson Road.
- 6) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |

42. PLANNING AREA 43: BUSINESS PARK

a. Descriptive Summary

Planning Area 43, as depicted in Figure III-25, provides for development of 21.8 acres devoted to Business Park uses.

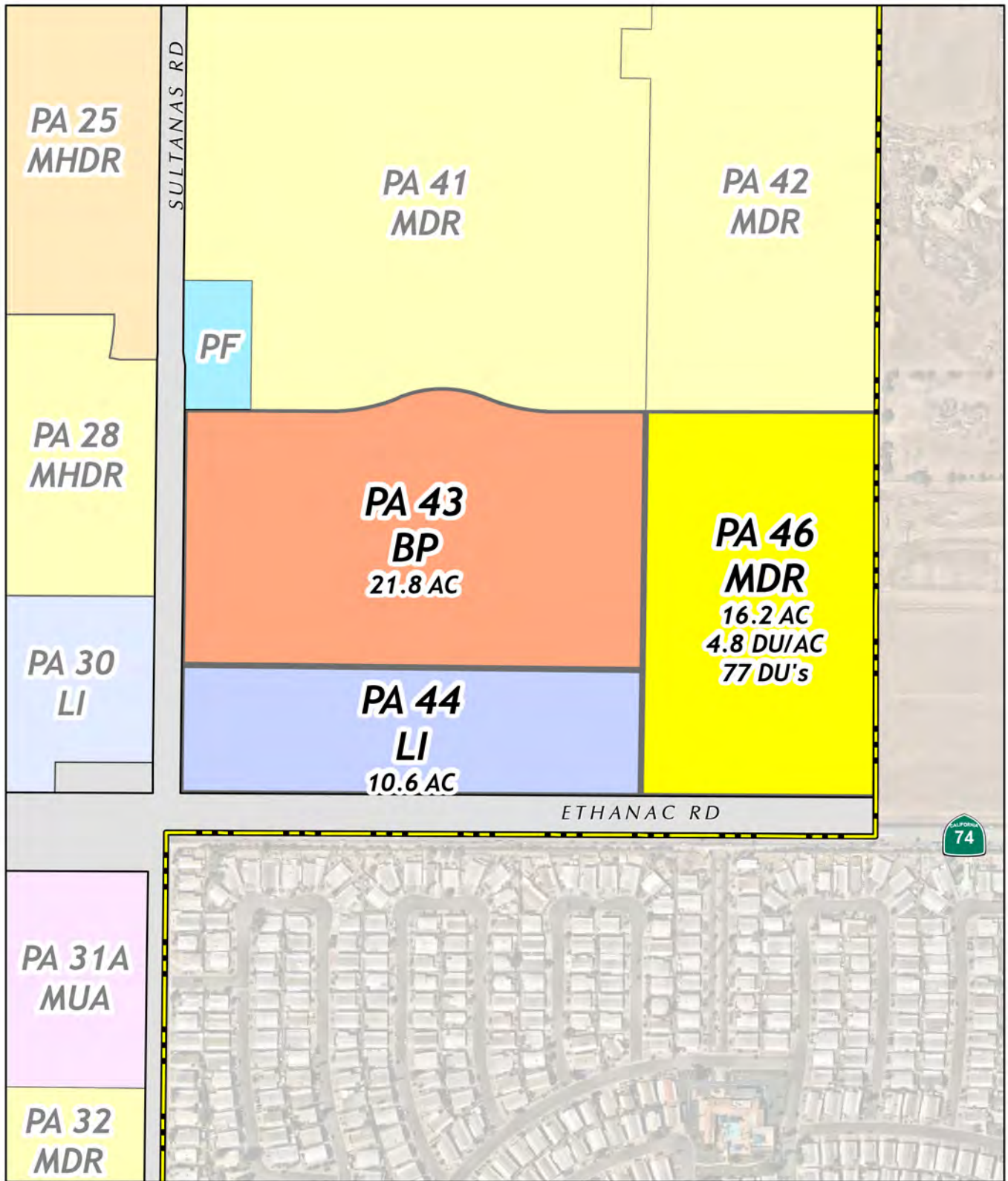
b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

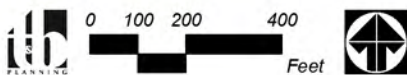
- 1) Primary access to Planning Area 43 shall be provided from Sultanas Road.
- 2) A minor intersection statement shall be developed at the intersection of Sultanas Road and the northwestern corner of Planning Area 43, as shown on Figure IV-4.
- 3) In conjunction with the first implementing plot plan submitted for this planning area, a conceptual site plan for the entire planning area shall be provided to the Riverside County Planning Department for informational purposes. This site plan shall show how the proposed plot plan integrates into the remainder of the planning area, to ensure proper design and layout of the parking and street access.
- 4) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 5) A special landscape treatment area shall be established between the business park uses in Planning Area 43 and the adjacent residential uses in Planning Area 41, as shown on Figure IV-12.
- 6) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |



Source(s): ESRI, Near Map Aerial (2021), RCTLMA (2021)

FIGURE III-25



PLANNING AREA 43, 44, & 46

III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

43. PLANNING AREA 44: LIGHT INDUSTRIAL

a. Descriptive Summary

Planning Area 44, as depicted in Figure III-25, provides for development of 10.6 acres devoted to Light Industrial uses.

b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

- 1) Primary access to Planning Area 44 shall be provided from Highway 74 (as approved by CalTrans), and Sultanas Road.
- 2) A project entry/intersection statement, as depicted on Figure IV-3, shall be developed at the intersection of Highway 74 and Sultanas Road.
- 3) Roadway landscape treatments, as illustrated in Figures IV-16 and 18, are planned along Highway 74 and Sultanas Road, respectively.
- 4) In conjunction with the first implementing plot plan submitted for this planning area, a conceptual site plan for the entire planning area shall be provided to the Riverside County Planning Department for informational purposes. This site plan shall show how the proposed plot plan integrates into the remainder of the planning area, to ensure proper design and layout of the parking and street access.
- 5) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 6) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |

44. PLANNING AREA 45: MEDIUM DENSITY RESIDENTIAL

a. Descriptive Summary

Planning Area 45, as depicted on Figure III-26, provides for development of 27.3 acres of Medium Density Residential uses with minimum lot sizes of 7,200 square feet. A maximum total of 115 dwelling units are planned at a target density of 4.2 du/ac.

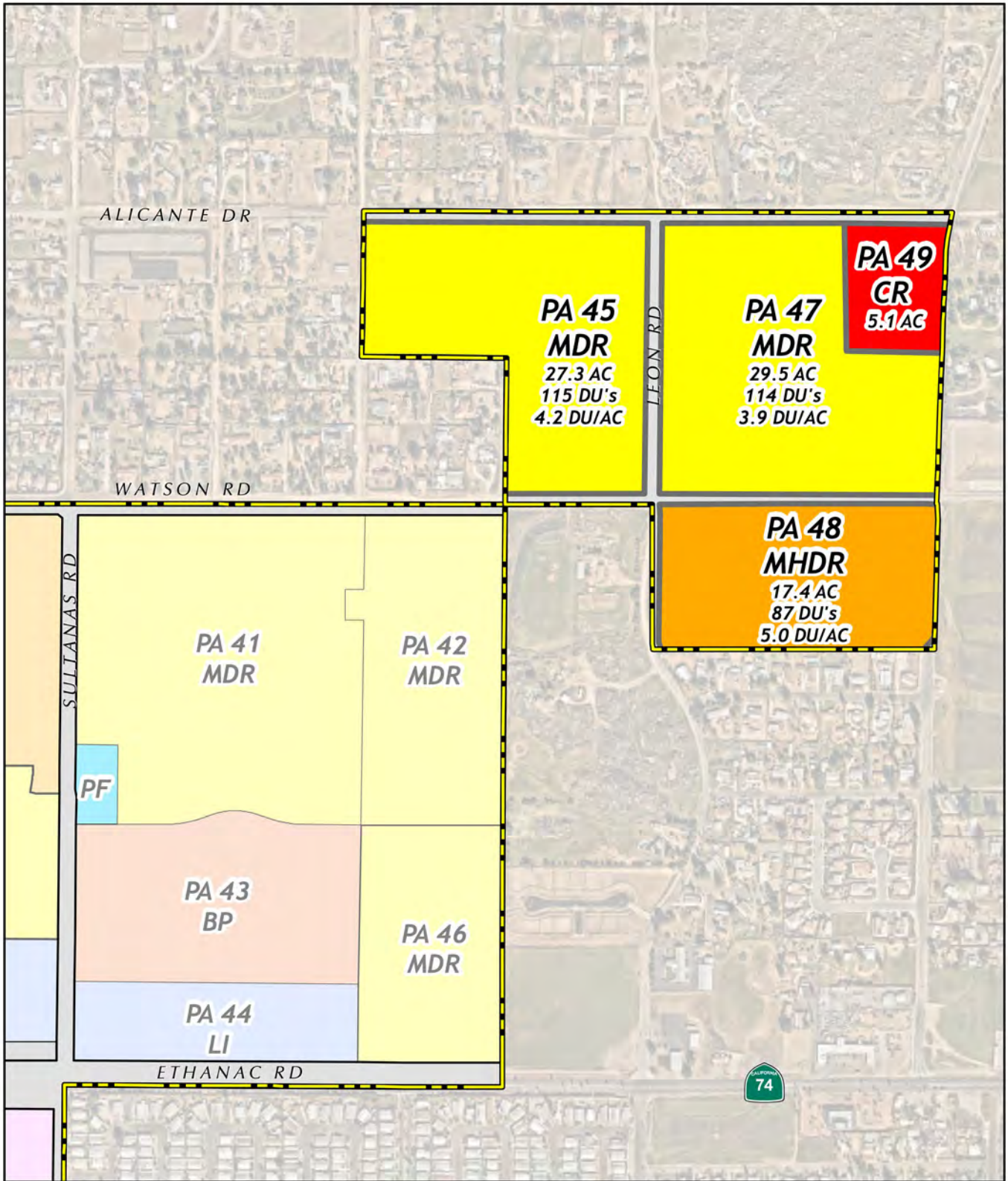
b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

- 1) Access to Planning Area 45 shall be provided from Watson Road, Leon Road, and Alicante Drive.
- 2) Roadway landscape treatments, as shown on Figure IV-18, shall be established along Watson Road.
- 3) A special landscape treatment, as shown on Figure IV-8, shall be provided along Watson Road to limit views of this planning area from adjacent uses.
- 4) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 5) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |



Source(s): ESRI, Near Map Aerial (2021), RCTLMA (2021)

FIGURE III-26



PLANNING AREA 45, 47, 48, & 49

III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

45. PLANNING AREA 46: MEDIUM DENSITY RESIDENTIAL

a. Descriptive Summary

Planning Area 46, as depicted on Figure III-25, provides for development of 16.2 acres of medium density residential uses with minimum lot sizes of 7,200 square feet. A maximum total of 77 dwelling units are planned at a target density of 4.8 du/ac.

b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

- 1) Access to Planning Area 46 shall be provided from Highway 74.
- 2) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 3) Roadway landscape treatment, as illustrated in Figure IV-15, is planned along Highway 74.
- 4) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |

46. PLANNING AREA 47: MEDIUM DENSITY RESIDENTIAL

a. Descriptive Summary

Planning Area 47, as depicted on Figure III-26, provides for development of 29.5 acres of Medium Density Residential uses with minimum lot sizes of 4,320 square feet. A maximum total of 11~~42~~ dwelling units are planned at a target density of 3.~~98~~ du/ac.

b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

- 1) Access to Planning Area 47 shall be provided from Watson Road and Leon Road.
- 2) Roadway landscape treatments, as shown on Figure IV-18, shall be established along Watson Road.
- 3) A special landscape treatment, as shown on Figure IV-8, shall be provided along Watson Road to limit views of this planning area from adjacent uses.
- 4) A mini-park may be developed within Planning Area 47, to be determined with implementing project(s).
- 5) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 6) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |

47. PLANNING AREA 48: MEDIUM HIGH DENSITY RESIDENTIAL

a. Descriptive Summary

Planning Area 48, as depicted on Figure III-26, provides for development of 17.4 acres of Medium High Density residential uses with minimum lot sizes of 3,780 square feet. A maximum total of 8~~78~~ dwelling units are planned at a target density of 5.~~04~~ du/ac.

b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

- 1) Access to Planning Area 48 shall be provided from Watson Road and Juniper Flats Road.
- 2) Roadway landscape treatments, as shown on Figure IV-18, shall be established along Watson Road.
- 3) A special landscape treatment, as shown on Figure IV-8, shall be provided along Watson Road to limit views of this planning area from existing adjacent residential uses.
- 4) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 5) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |

48. PLANNING AREA 49: COMMERCIAL RETAIL

a. Descriptive Summary

Planning Area 49, as depicted in Figure III-18, provides for development of 5.1 acres devoted to Commercial Retail uses.

b. Land Use and Development Standards

Please refer to Ordinance No. 348. (See Specific Plan Zone Ordinance Tab.)

c. Planning Standards

- 1) Primary access to Planning Area 49 shall be provided from Juniper Flats Road and Alicante ~~Road~~ Drive.
↳ Any proposed development of the Planning Area shall require approval of a Plot Plan by the Director of Planning or Planning Commission (as applicable) in accordance with requirements of Section 18.30 of Ordinance 348. The design of structures and the broader property shall demonstrate effective integration and connectivity with the other Planning Areas, including (but not limited to) identifying signage and landscaping.
- 2) ~~In conjunction with the first implementing plot plan submitted for this planning area, a conceptual site plan for the entire planning area shall be provided to the Riverside County Planning Department for informational purposes. This site plan shall show how the proposed plot plan integrates into the remainder of the planning area, to ensure proper design and layout of the parking and street access.~~
- 2)3) Please refer to Section IV, Design Guidelines, for design-related criteria.
- 3)4) Please refer to Section III.A. for the following Development Plans and Standards that apply site-wide:

- | | |
|---|---|
| III.A.1: Specific Land Use Plan | III.A.6: Grading Plan |
| III.A.2: Circulation Plan | III.A.7: Landscaping Plan |
| III.A.3: Drainage Plan | III.A.8: Public Facilities Phasing Plan |
| III.A.4: Water and Sewer Plans | III.A.9: Phasing Plan |
| III.A.5: Open Space and Recreation Plan | |

IV. DESIGN GUIDELINES

A. PURPOSE AND INTENT

Generally speaking, these guidelines have been devised as a method for achieving a high quality, cohesive design fabric for the community that will develop within the *MENIFEE NORTH* Specific Plan area.

More specifically, the purpose of these Design Guidelines is:

- To provide the County of Riverside with the necessary assurance that the Specific Plan area will develop in accordance with the quality and character proposed herein;
- To provide guidance to developers, builders, engineers, architects, landscape architects and other professionals in order to maintain the desired design quality;
- To provide guidance to County staff, the Planning Commission and the Board of Supervisors in the review of future development projects in the Specific Plan area;
- To provide guidance in the formulation of CC&Rs for the use of land in the Specific Plan area;
- To provide guidance in the formulation of concise development guidelines for the various planning areas within the Specific Plan boundaries.
- To provide development guidelines which permit the *MENIFEE NORTH* Specific Plan area to develop its own theme and character while allowing it to interface and respond to the character and design fabric of adjacent specific plan areas which may be currently under review or are proposed in the future.

The Design Guidelines provided herein are intended to function as a living document. They are subject to modification over time to allow for response to anticipated conditions, such as changes in taste, community desires and the marketplace, or significant changes within adjacent specific planning areas.

B. GENERAL GUIDELINES

As demonstrated by the *Conceptual Landscape Plan*, Figure IV-1, careful thought has been given to integrate the structural and aesthetic elements of a balanced, active community. To ensure that this carefully considered plan is implemented in a manner that will bring *MENIFEE NORTH* a sense of its own character, a central theme has been established, as described below.

1. PROJECT THEME

The underlying design concept and theme for *MENIFEE NORTH* incorporates a suburban corridor along the highly visible Highway 74, transitioning outward with adjacent compatible land uses, to a more rural project edge. The project provides for unifying elements in each planning area. Streetscape corridors, entry monumentation, and project-wide design guidelines will provide unifying characteristics to strengthen the overall theme. Additionally, the project will create a community identity through control of elements such as architecture, landscaping, color palette of building materials, paving, community theme walls, fencing, lighting, street furniture, signage and graphics.

Landscaping plays a significant role in establishing *MENIFEE NORTH's* distinctive character. The project's mixed use concept seeks to develop an association of different land uses by utilizing landscaping, which has been designed to strengthen the overall theme of the development, through the use of specific plant and hardscape materials, while still allowing for specific land use individuality.

2. PROJECT SETTING

a. Preservation of Natural Open Space and Hillside

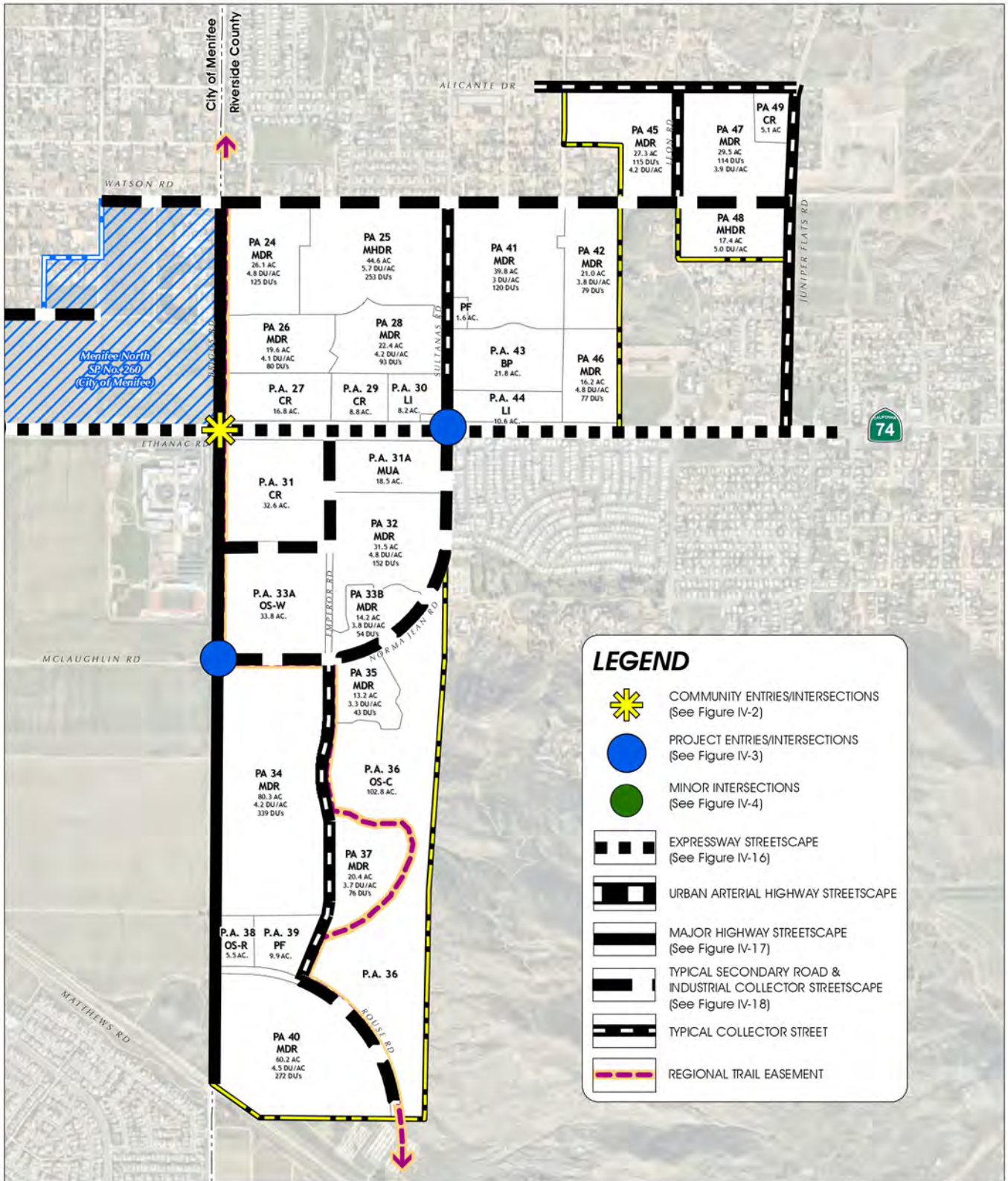
A significant amount of natural open space exists east of Briggs Road and south of Highway 74. This area is located in hillside portions of the project and can be seen from several areas of the surrounding valley. A key aspect of the *MENIFEE NORTH* Specific Plan is the preservation of this hillside area, which accounts for approximately 103 acres.

b. Development of Improved Recreational Open Space

The *MENIFEE NORTH* Specific Plan proposes the integration of recreational trails, community parks, a public park, mini-parks and school playgrounds to create a master recreational program which provides adequate areas to accommodate active recreational needs of the future residents of the site.

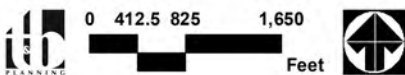
c. Creation of an Integrated Land Use Concept

In designing the land use plan for *MENIFEE NORTH*, it was intended that several opportunities be created as a means to provide a sense of arrival and to establish the overall theme. A hierarchy was developed which would establish this arrival theme, and which would provide a feeling of transition from varying intensity of land use. This hierarchy consists of Community Entries and Intersections, Project Entries and Intersections, Minor Intersections and Neighborhood Entries.



Source(s): ESRI, Near Map Aerial (2021), RCTLMA (2021)

FIGURE IV-1



CONCEPTUAL LANDSCAPE PLAN

III. SPECIFIC PLAN

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

C. COMMUNITY ELEMENTS

The *Conceptual Landscape Plan*, Figure IV-1, consists of community elements that reflect continuity throughout the project. Community elements such as entries, intersections, streetscapes, walls, fences and land use transitions will serve as a unifying element. Individually, these elements identify with specific land use, however through the consistent use of similar forms and materials, are designed to be a reinforcement of the overall project theme.

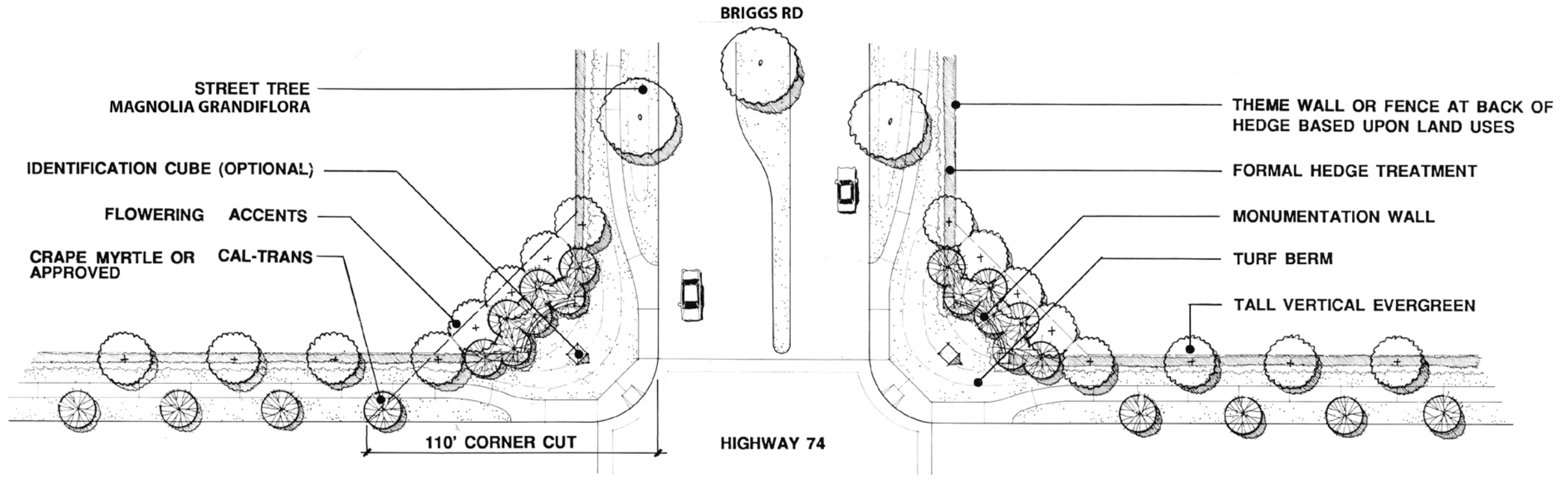
1. PROJECT MONUMENTATION TREATMENTS

Project monumentation is proposed at key entries and intersections which will become major focal points of the project's identification. Monumentation treatments in some situations share the same level of hierarchy, however must identify and be compatible with varied land uses. For this reason the forms, size and materials of these treatments have been carefully designed to be compatible with residential as well as non-residential uses. A hierarchy of monumentation statements, as discussed generally above and detailed below, has been established for *MENIFEE NORTH*.

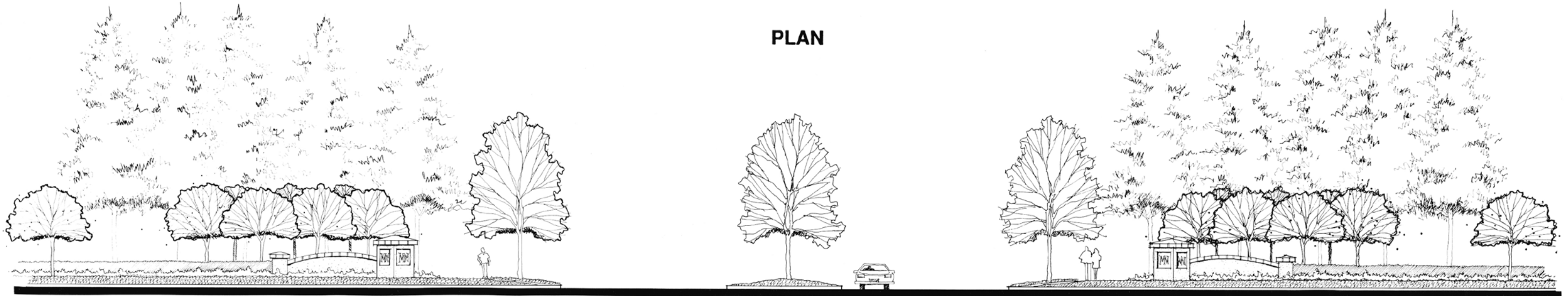
a. **Community Entries and Intersections** (Figure IV-2)

Community entries, as illustrated in Figure IV-2, occur at Arterial Highway and Major Highway intersections, and are *MENIFEE NORTH*'s "Gateway" to business and residential communities. These entries will serve to visually and physically announce major arrival points within *MENIFEE NORTH*. The overall sense of entry is created by a harmonious blend of thematic features that include:

- A 110-foot corner cutoff monumentation treatment.
- A 22-foot turf grass threshold introducing the monumentation treatment.
- Freestanding community identification cube compatible with community theme walls.
- A 25-foot radius monumentation wall, compatible with community theme walls, back up identification cube.
- Small flowering accent trees, which match the trees at Highway 74, in a formal grouping to provide color and to reinforce the overall streetscape theme.
- Tall evergreen trees behind flowering accent trees to provide a solid visual backdrop and to reinforce the overall streetscape theme.
- Formal hedge row treatment with a foreground of flowering ground cover to define physical limits of the entry statement and to provide screening of parked cars in commercial areas.
- Community entries may incorporate community theme walls or fencing at the back of landscape development zone limits, depending on adjacent land use requirements. See Figure IV-2.



PLAN



SECTION

FIGURE IV-2

Not to Scale

COMMUNITY ENTRIES AND INTERSECTIONS

IV. DESIGN GUIDELINES
MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

Community intersections are intended to occur in conjunction with community entries, where identification is not necessary or compatible with adjacent land uses. Intersections may incorporate community theme walls or fencing depending on adjacent land use requirements. See Figure IV-2.

b. Project Entries and Intersections (Figure IV-3)

Project entries, as illustrated in Figure IV-3, are the second level of hierarchy for monumentation, and occur at intersections of Arterial Highways and Major Highways. Project entry statements occur in a formal symmetrical configuration on both sides of the street and announce arrival to individual business or residential communities. Thematic features include:

- A 95-foot corner cutoff monumentation treatment.
- A 22-foot turf grass threshold introducing the monumentation treatment.
- A 25-foot radius monumentation wall, identifying the project area by name. Graphics and materials are to be compatible with the overall community theme.
- Formal grouping of tall evergreen trees to provide continuity with community entries.
- Formal hedge row treatment, duplicating the appearance of community entries.
- Project entries may incorporate community theme walls or fencing at the back of landscape development zone limit depending on adjacent land use requirements. See Figure IV-3.

Project intersections are to be used in conjunction with project entries where continuity is desired, however project identification is not. These monumentations share the same thematic features with the exception of signage graphics. Intersections may incorporate a community theme wall or fencing, depending on adjacent land use requirements.

c. Minor Intersections (Figure IV-4)

Minor intersections, as illustrated in Figure IV-4, are the third level of hierarchy for *MENIFEE NORTH*. Treatments reflect similar thematic features of previously described monumentations as follows:

- A 60-foot corner cutoff monumentation treatment.
- A 20-foot turf grass threshold.
- Formal grouping of tall evergreen trees to provide continuity with community and project monumentation treatments.
- Formal hedge row treatment compatible with other monumentation.

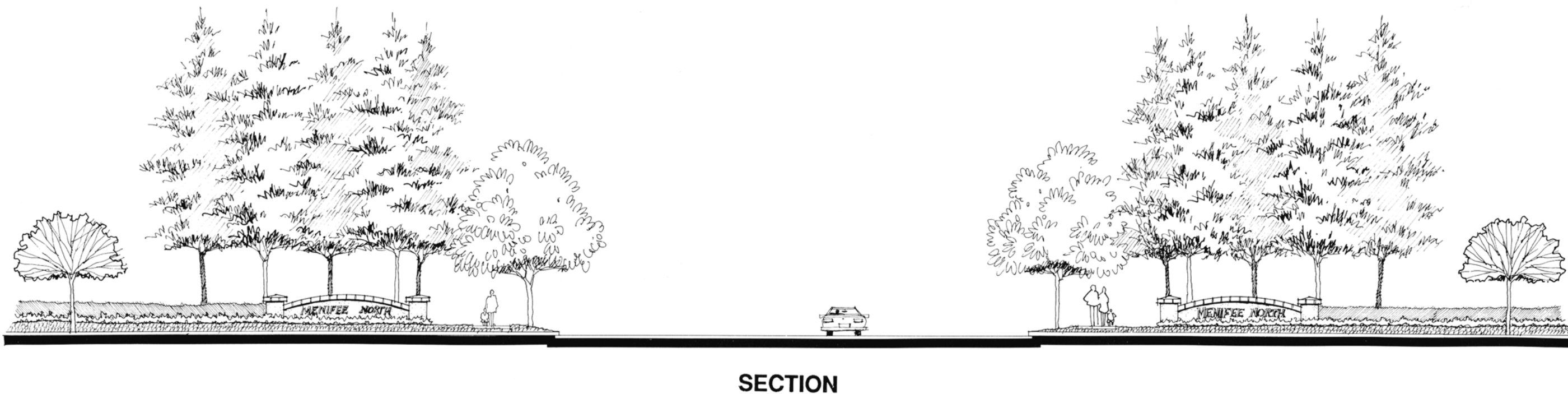
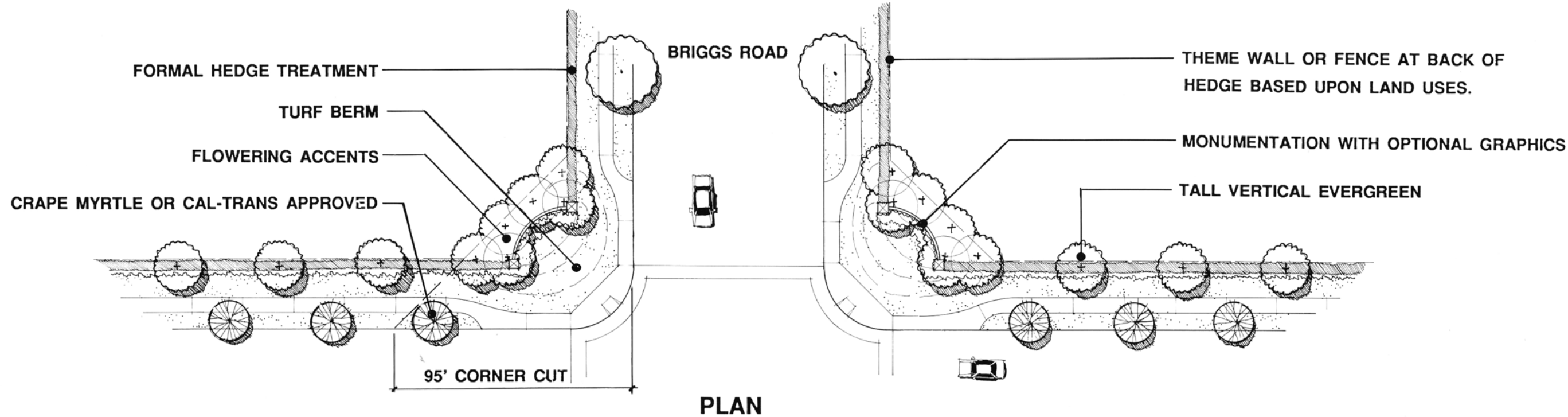
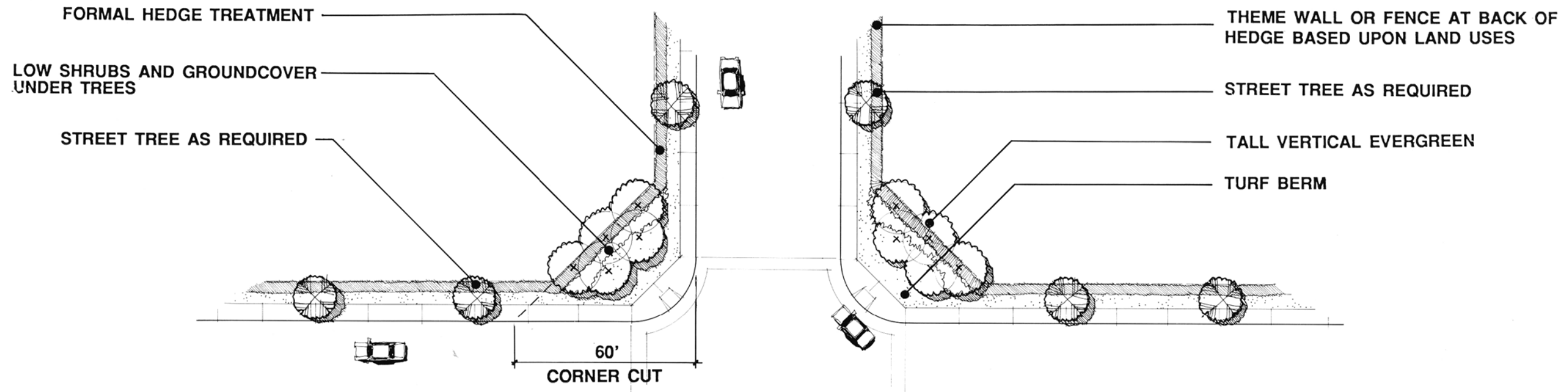


FIGURE IV-3

th **Not to Scale**

PROJECT ENTRIES AND INTERSECTIONS

IV. DESIGN GUIDELINES
 MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4



PLAN



SECTION

FIGURE IV-4

Not to Scale

MINOR INTERSECTIONS

IV. DESIGN GUIDELINES

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

- Intersections may incorporate community theme walls or fencing, depending on adjacent land use requirements. See Figure IV-4.

d. Neighborhood Entries / Business Monument Signage (Figure IV-5 and Figure IV-6)

Neighborhood Entries (see Figure IV-5) and Business Monument Signage (see Figure IV-6) share the fourth level of hierarchy and complete the monumentations for *MENIFEE NORTH*. These treatments are intended for future local streets that have not been identified on Figure IV-1, *Conceptual Landscape Plan*, but will occur as part of the actual project design. Individual users are encouraged to reflect forms, materials, and graphics of other project monumentations to provide overall continuity and strengthen the theme of *MENIFEE NORTH*'s community elements.

2. LAND USE TRANSITIONS

In an attempt to create a high-quality, environmentally sensitive and aesthetically pleasing community, the *MENIFEE NORTH* Specific Plan proposes different types of transition treatments. These treatments will provide environmental design and land use edge treatments which facilitate a mixed use concept. They will also allow for land uses of similar intensity or compatibility to interlink, while allowing land uses of dissimilar intensity or limited compatibility to be physically separated or buffered from each other. The land use transitions, interfaces and edge treatments proposed for *MENIFEE NORTH* are intended to:

- Provide for the separation and buffering of dissimilar land uses or residential densities by employing community design elements.
- Ensure proper treatment of sensitive areas, such as open space and hillsides.
- Establish design criteria for creating effective linkages between natural and improved open space and residential, industrial, commercial, business park and educational uses.

The recommended locations for each of these land use transitions are detailed in the *Planning Area Development Standards* (Section III.B). A description of each transition is provided below.

a. On-Site Residential / Adjacent Existing Residential Interface (Figure IV-7)

This interface occurs along the northern and eastern portions of the project. This interface is created where proposed low, medium and medium-high density residential land uses within *MENIFEE NORTH* are adjacent to existing off-site rural properties. Figure IV-7 illustrates the on-site residential/existing adjacent residential land use transition which incorporates the following guidelines:

1. To retain the rural nature of these interfaces, a 40-foot backyard setback is provided for on-site residential lots to create a landscaped buffer.
2. Encourage on-site residential lots to establish generous backyard landscaping with informal groupings of both vertical and canopy trees to limit views from adjacent existing residential properties to proposed residential roof lines.

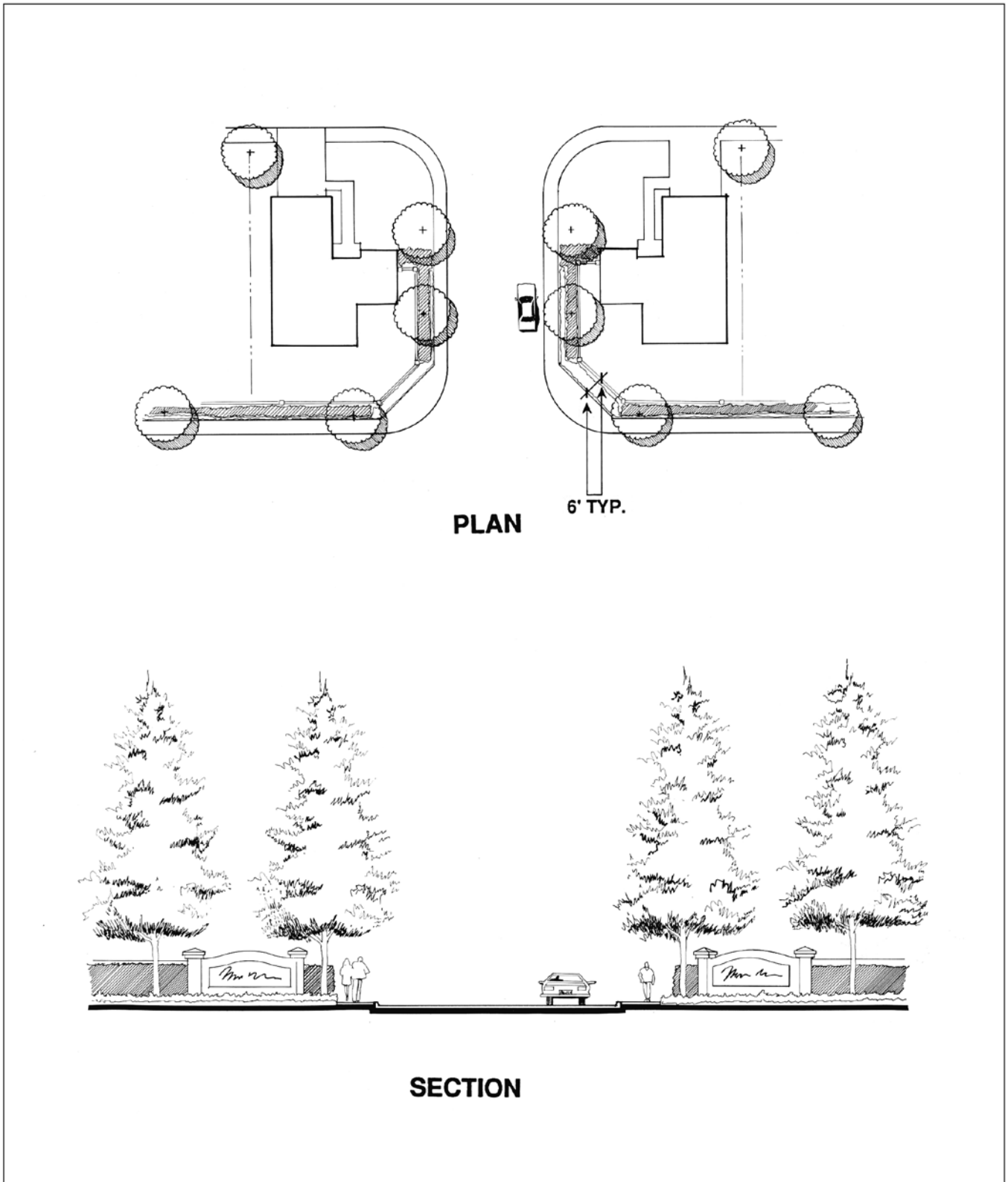



FIGURE IV-5


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NEIGHBORHOOD ENTRIES

IV. DESIGN GUIDELINES

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

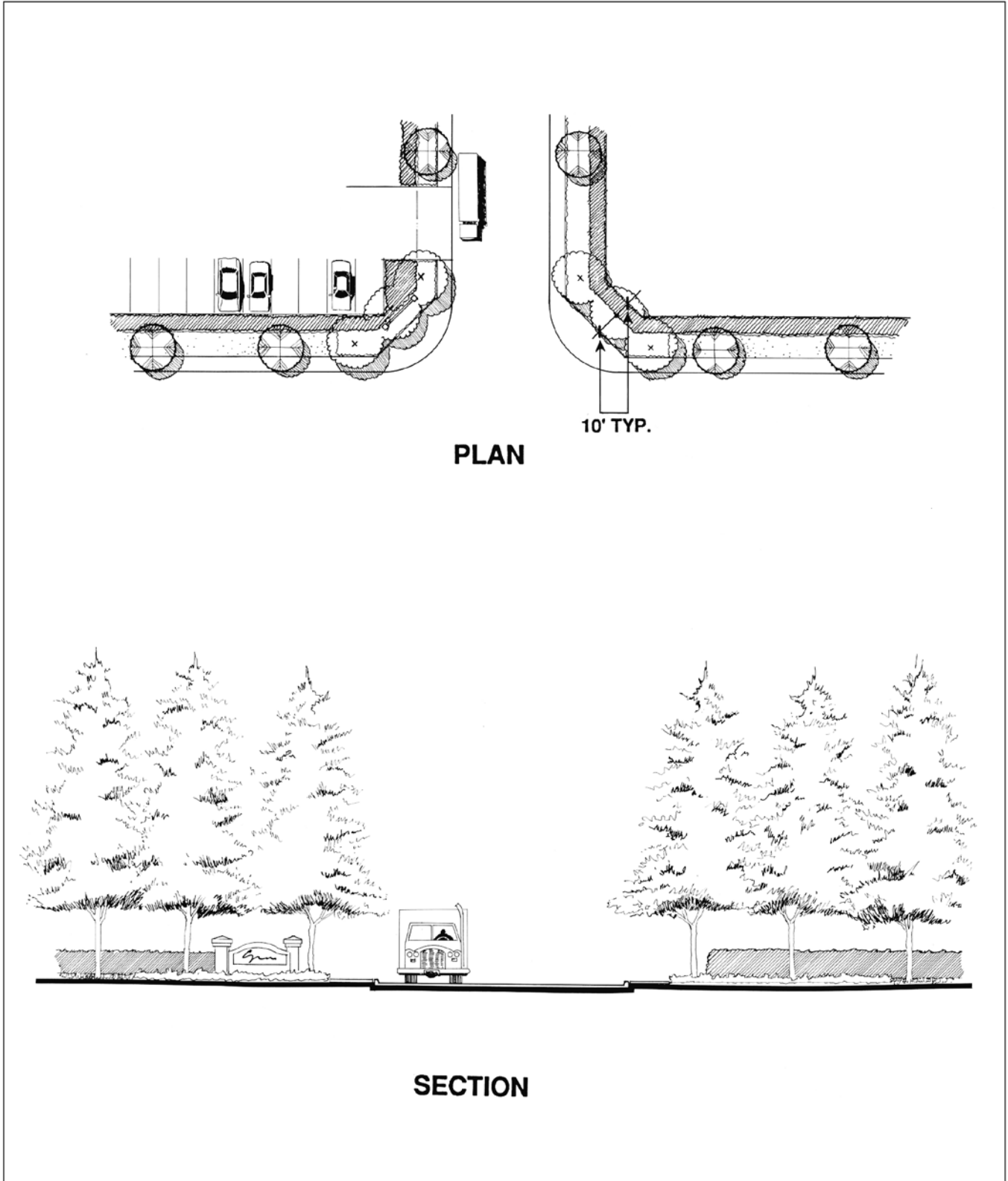



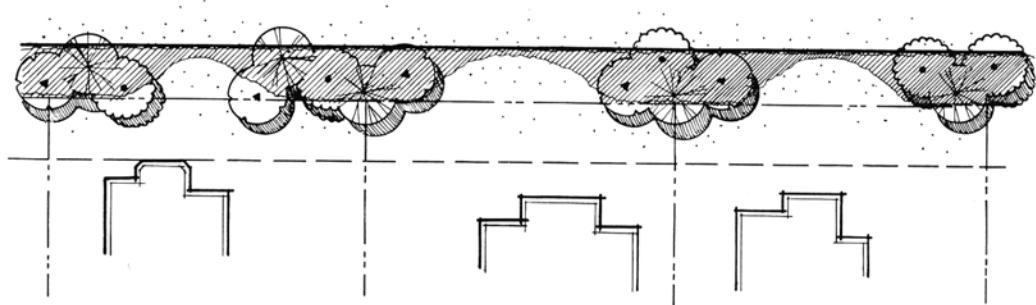
FIGURE IV-6


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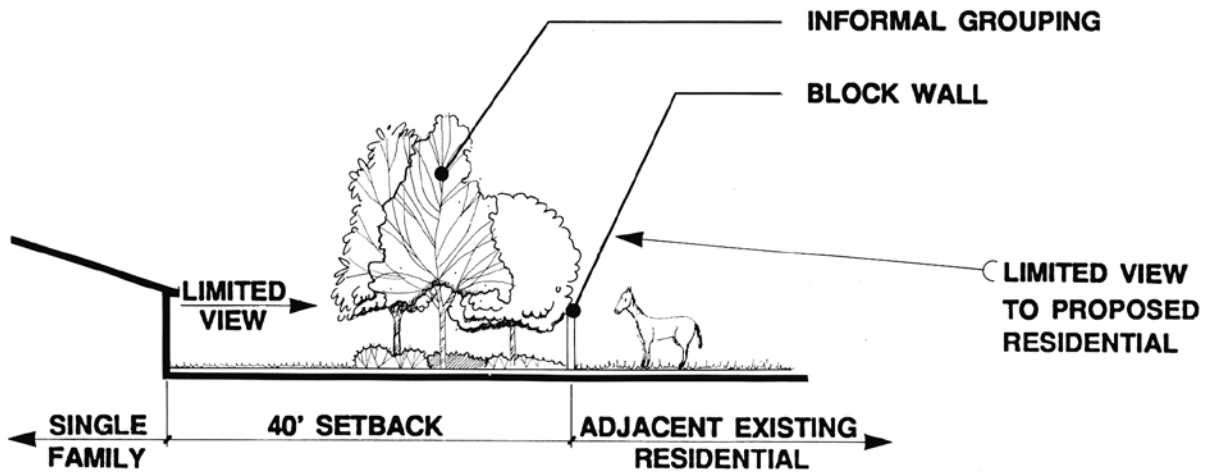
BUSINESS MONUMENT SIGNAGE

IV. DESIGN GUIDELINES

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4



**PROPOSED SINGLE FAMILY
PLAN**



SECTION

FIGURE IV-7



Not to Scale

**ON-SITE RESIDENTIAL/
ADJACENT EXISTING RESIDENTIAL INTERFACE**

IV. DESIGN GUIDELINES

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

3. A 6-foot high community theme wall shall be provided at the property line.
4. Where grading changes occur, incorporate contour grading techniques, avoiding sharp cut manufactured slopes to create a more natural appearance.

b. Secondary Road Streetscape / Adjacent Existing Residential Interface (Figure IV-8)

This land use transition is created where streetscape improvements for Watson Road or Juniper Flats Road are adjacent to adjacent existing rural properties. The 88-foot street right-of-way would separate on-site residential and school land uses from off-site uses. This land use transition, as illustrated in Figure IV-8, incorporates the following guidelines:

1. Watson Road and Juniper Flats Road shall incorporate an 11-foot landscape development zone with informal groupings of trees and small flowering accents to retain a rural feeling while limiting views.
2. On-site residential lots are encouraged to provide generous backyard landscaping with informal groupings of trees to limit views from adjacent existing residential properties to proposed residential roof lines.
3. A 6-foot high community theme wall shall be provided at the property line of residential lots and school property abutting the Watson Road or Juniper Flats Road right-of-way.

Along portions of Watson Road, a channel right-of-way further separates on-site residential and school uses from off-site development, limiting view potential.

c. Drainage Channel / Residential, School or Park Interface (Figure IV-9)

There are several areas within the *MENIFEE NORTH* project where drainage channels interface with residential uses and school and park property. Generally these channels are located adjacent to Watson, Briggs and McLaughlin Roads, between the street right-of-way and residential lots, a school or park area. This interface is illustrated on Figure IV-9 and incorporates the following guidelines:

1. Streetscape landscaping between the street curb and the back of the landscape development zone shall be as required by the *Conceptual Landscape Plan*, Figure IV-1.
2. A 6-foot high community theme wall shall be provided at the streetscape side of the channel and a 6-foot high community theme fence shall be provided at the residential, park or school side of the channel. Figure IV-21 illustrates the *Community Walls and Fencing Plan* for *MENIFEE NORTH*.
3. Generous residential backyard setbacks are encouraged to allow for appropriate landscape buffers on individual lots.

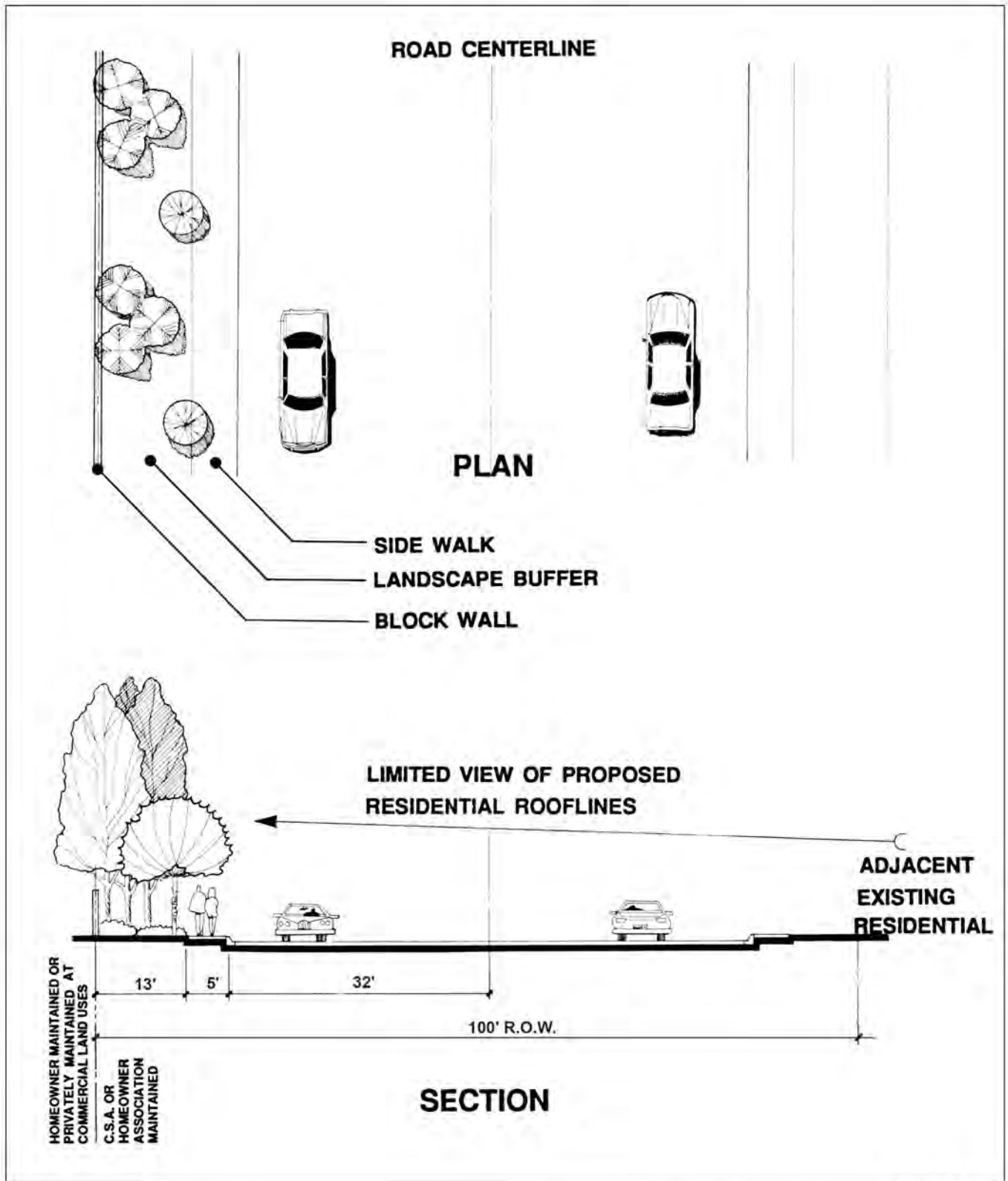


FIGURE IV-8

**SECONDARY ROAD STREETSCAPE/
ADJACENT EXISTING RESIDENTIAL INTERFACE**

IV. DESIGN GUIDELINES

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4



Not to Scale

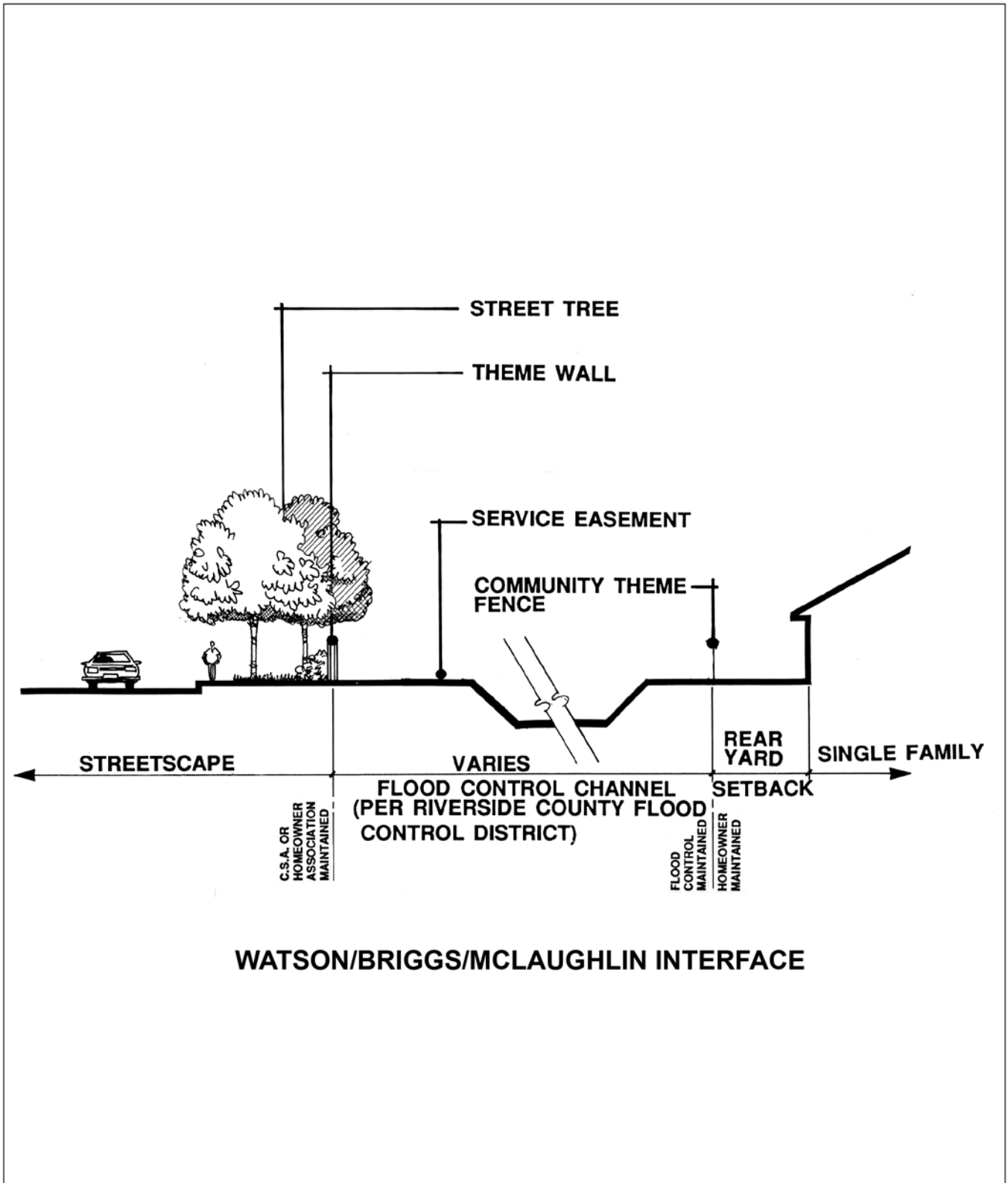
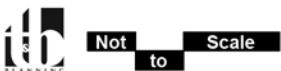


FIGURE IV-9

**DRAINAGE CHANNEL/
RESIDENTIAL, SCHOOL, OR PARK INTERFACE**

IV. DESIGN GUIDELINES

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4



d. Drainage Channel / Commercial Retail Interface (Figure IV-10)

Edge conditions created where Commercial Retail land uses interface with the flood control channel adjacent to Briggs Road is illustrated in Figure IV-10 and incorporates the following guidelines:

1. Streetscape landscaping between the street curb and the back of the landscape development zone shall be as required by the *Conceptual Landscape Plan*, Figure IV-1.
2. A 6-foot high community theme wall shall be provided at both sides of channel. Figure IV-21 illustrates the *Community Walls and Fencing Plan* for *MENIFEE NORTH*.
3. Site plans for Commercial Retail uses should strive to create reverse frontage products at this edge condition, however allowing for signage on the rear of buildings for good visibility from Briggs Road. Signage and graphics should be compatible with the overall community theme.

e. Residential / Park or School Interface (Figure IV-11)

At edge conditions where proposed park improvements are adjacent to proposed and existing single family residential land uses, the following guidelines apply:

1. A 6-foot high combination low stucco wall with tubular steel fence shall be provided at the property line to allow open spacious view into park from adjacent residential uses, while limiting public intrusion into private lots.
2. A 20-foot wide landscaped buffer shall be established at the park edge.
3. Landscape tree plantings for the park site should stress the use of tall skyline tree plantings to retain views into the park site. Trees such as Eucalyptus species, plane trees and California Sycamores should be planted in random informal groves or groupings. Tree plantings should generally be concentrated along the outer edges of the park to allow for the maximization of unrestricted usable open space.
4. Site plans for adjacent residential land uses should strive to provide direct, convenient circulation connections to park site.
5. Encouraged graceful mounding should be designed throughout park to create a natural appearance.

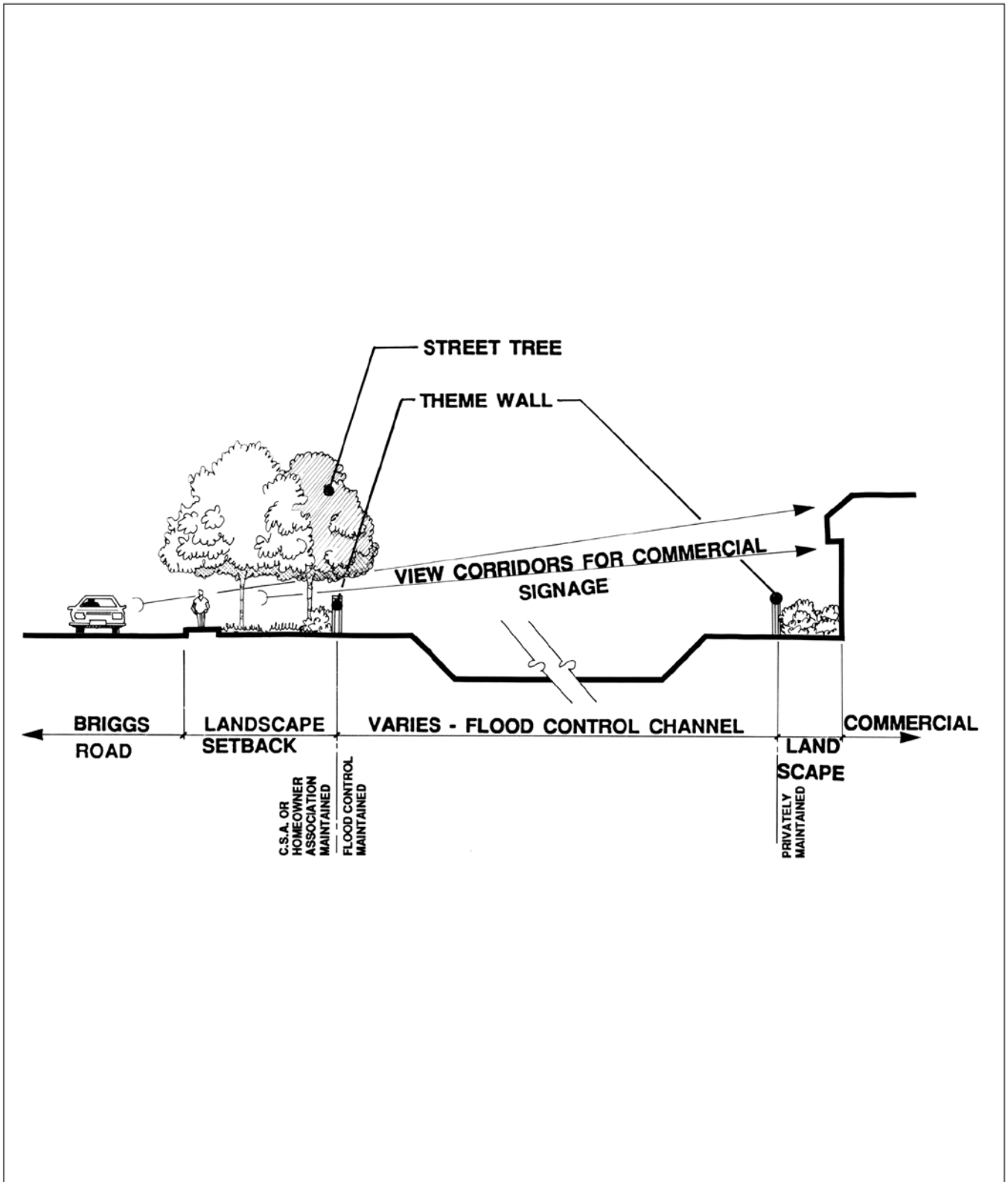
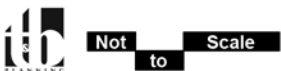


FIGURE IV-10

**DRAINAGE CHANNEL/
COMMERCIAL RETAIL INTERFACE**

IV. DESIGN GUIDELINES

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4



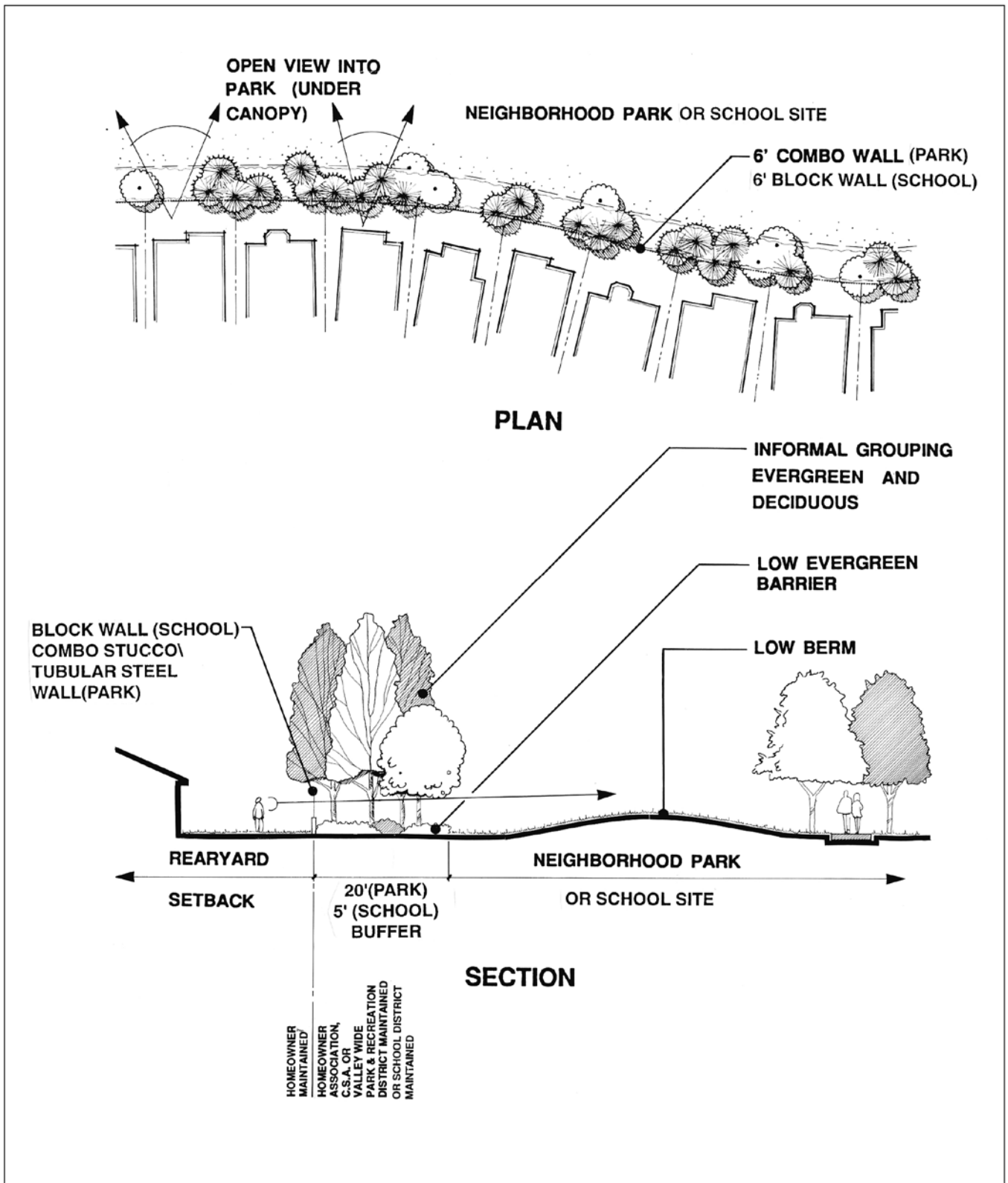


FIGURE IV-11



Not to Scale

RESIDENTIAL/PARK OR SCHOOL INTERFACE

IV. DESIGN GUIDELINES

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

At conditions created where proposed school improvements are adjacent to residential properties, the following guidelines apply:

1. A 6-foot high solid block wall which is compatible in appearance with community theme wall shall be provided at the property line.
2. A minimum 5-foot wide planting area should be provided on the school yard side of the property line.
3. Landscape tree plantings for the outdoor recreation areas of the school should stress the use of tall skyline tree plantings.

g. Residential / Business Park, Mixed-Use Planning Area, Light Industrial, or Commercial Retail Interface (Figure IV-12)

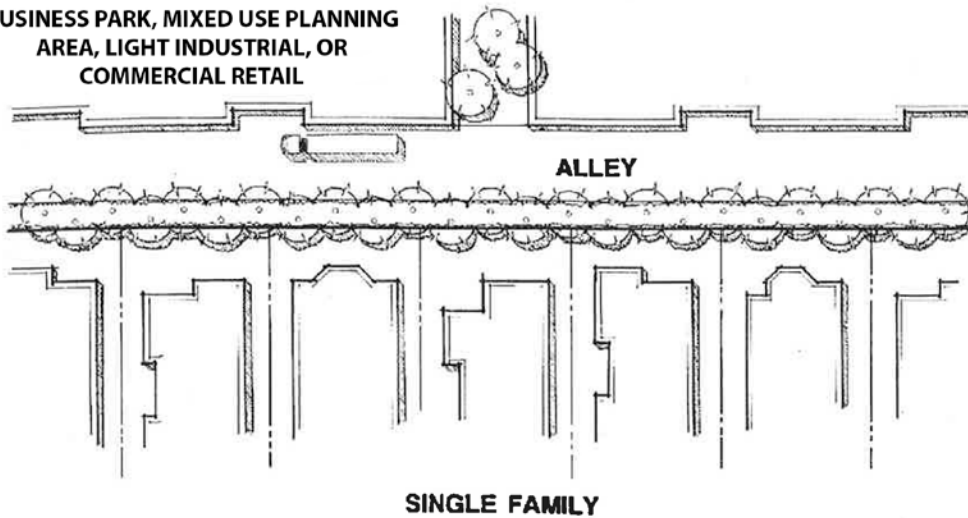
This interface, shown in Figure IV-12 occurs where residential land uses, either existing or proposed, are adjacent to Business Park, Mixed Use Planning Area, Light Industrial, or Commercial Retail land uses. The following guidelines apply:

1. Site plans should strive to orient parking, vehicular circulation and loading and storage areas away from the residential interface. Where parking, vehicular circulation, loading or storage areas occur adjacent to residential areas, they should be separated from the residential areas by a minimum 10-foot wide landscaping screening buffer.
2. A 10-foot wide planting area shall be incorporated along the business park, light industrial, commercial retail, or mixed use planning area side of the screen wall.
3. Screening shall be provided at the commercial retail/business park/light industrial/mixed-use planning area edge by a 6 foot high decorative wall which is compatible in appearance with the community walls.
4. Commercial retail, business park, light industrial and mixed-use planning areas visible from adjacent residential lots shall be liberally and informally planted with a combination of vertical trees, canopy trees, shrubs, and groundcovers.
5. Site plans for single family lots should strive to construct units a minimum of 15 feet from the property line adjacent to the transition area.

h. Residential / Regional Trail / Open Space Interface (Figure IV-13)

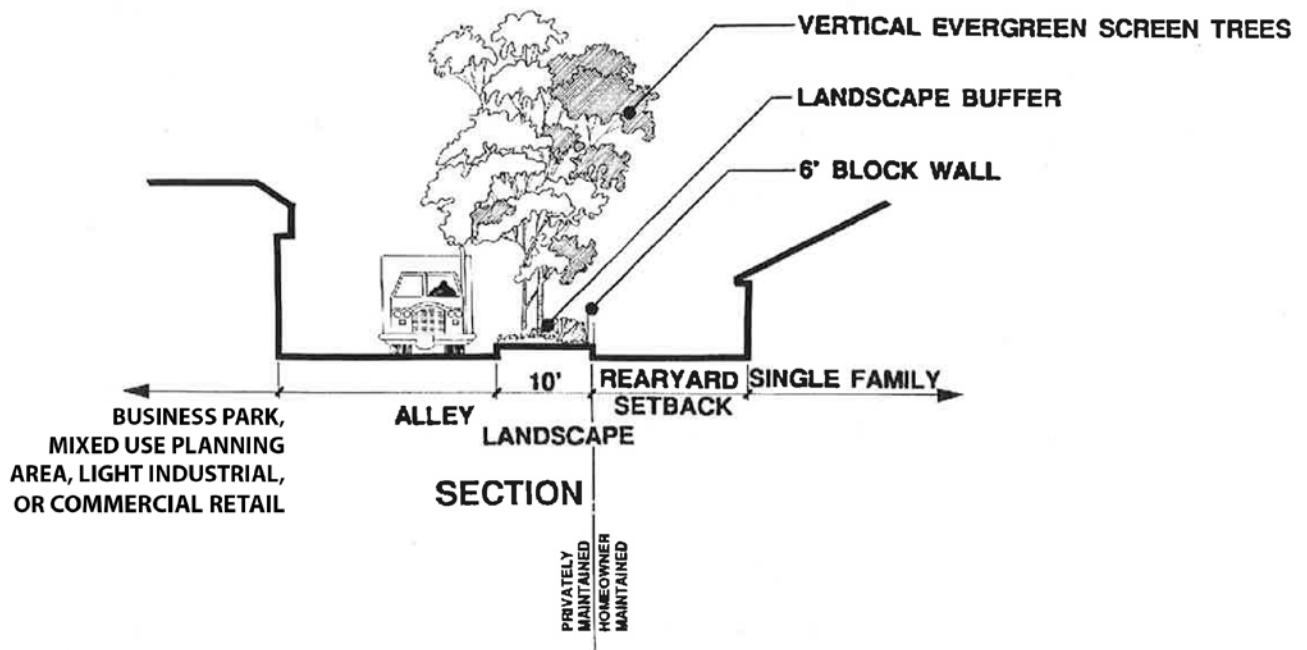
This interface occurs where residential planning areas are adjacent to the regional trail and open space areas. In areas where grading will be required at up-hill natural slopes, a fuel modification interface shown on Figure IV-13, is proposed. This treatment will ensure proper revegetated edge conditions, provide for an aesthetic transition

BUSINESS PARK, MIXED USE PLANNING
AREA, LIGHT INDUSTRIAL, OR
COMMERCIAL RETAIL



SINGLE FAMILY

PLAN



BUSINESS PARK,
MIXED USE PLANNING
AREA, LIGHT INDUSTRIAL,
OR COMMERCIAL RETAIL

SECTION

FIGURE IV-12



Not to Scale

**RESIDENTIAL/BUSINESS PARK, MIXED USE PLANNING AREA,
LIGHT INDUSTRIAL, OR COMMERCIAL RETAIL INTERFACE**

IV. DESIGN GUIDELINES

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

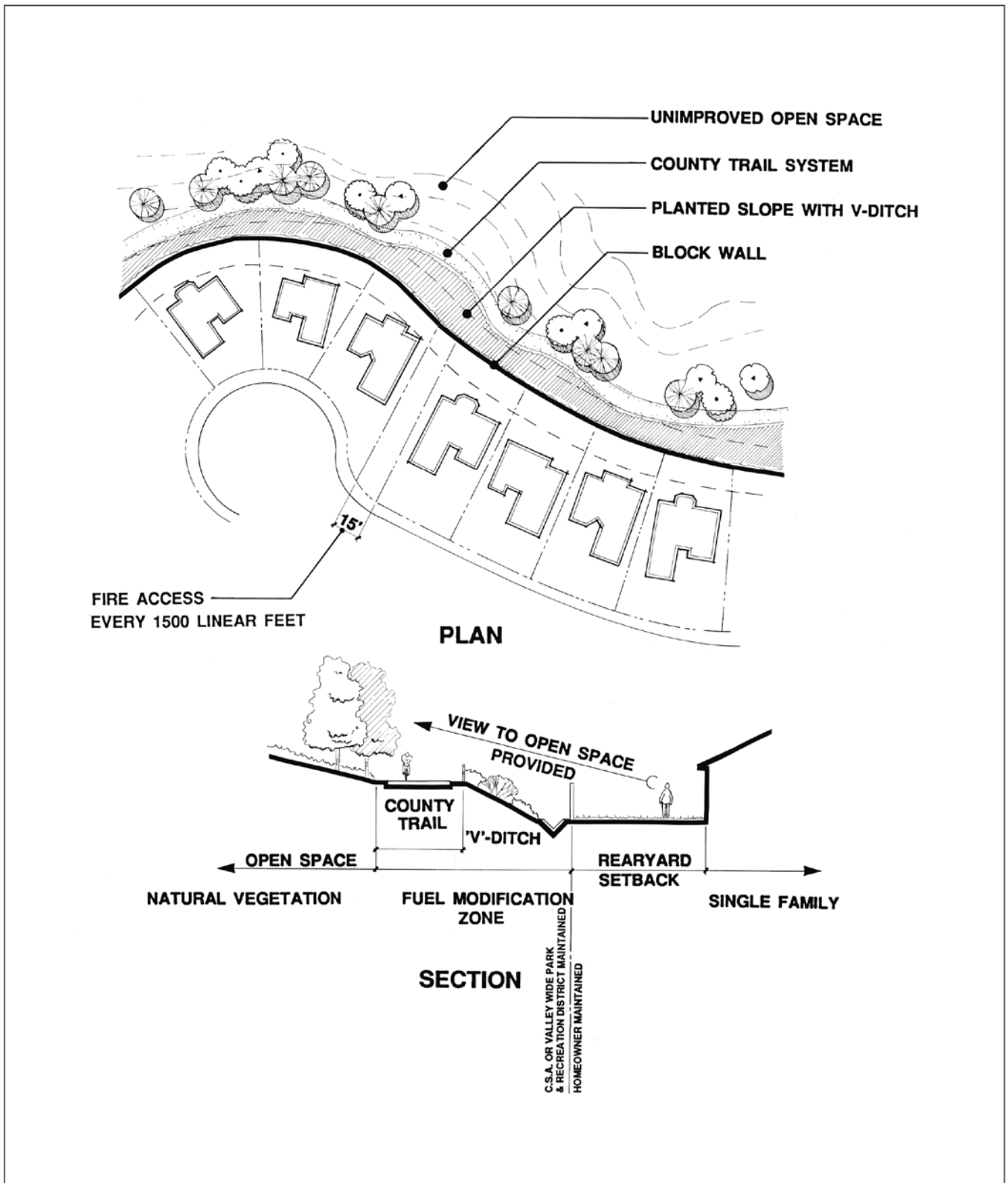
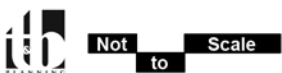


FIGURE IV-13

**RESIDENTIAL/REGIONAL TRAIL/
OPEN SPACE INTERFACE**

IV. DESIGN GUIDELINES

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4



between natural and created conditions and if, and when necessary, reduce fire hazards. Per the Riverside County Fire Department the following guidelines apply:

1. A 6-foot high solid masonry wall shall be provided at the rear of residential lots.
2. Masonry walls shall provide for access openings for fire equipment every 1,500 linear feet.
3. All areas disturbed by grading shall be revegetated with fire retardant plants subject to Riverside County Fire Departments approval.
4. Any trees planted shall be located a minimum of 30 feet from the rear of residential property lines.

i. Regional Trail / Roadway Interface (Figure IV-14)

A portion of the regional trail will be located along project roadways (Briggs Road, McLaughlin Road, Rouse Road, and Emperor Road). In these areas, adequate buffering is required to separate the trail from the adjacent right-of-way. This interface is depicted in Figure IV-14 and incorporates the following guidelines:

1. An 8-foot wide planting area shall be provided between the pedestrian sidewalk and the regional trail. Canopy street trees shall be planted in this area in accordance with the *Conceptual Landscape Plan*.
2. A 2-foot wide planting area to accommodate shrubs and groundcover shall be provided between the regional trail and the adjacent land uses.
3. Where residential lots abut the regional trail, a project theme wall shall be constructed at the property line.

j. Community Park/School Shared Use Interface (Figure IV-15)

This interface, as shown in Figure IV-15, occurs where elementary school land uses abut improved open space parks. The main objective of this interface will be to emphasize and facilitate a "Shared Use" concept. A shared use concept implies that there would be times at which it would be advantageous for the school to utilize the park for school activities such as athletics, while it might be advantageous for the community to use certain outdoor facilities within the school site such as ball courts or playground equipment during and/or after school hours. To implement this idea of shared use, the following guidelines should apply to the park/school interface.

1. Direct physical barriers such as walls or fences should not be placed between the park and school. Separation should be implied by using rolling mounds and landscaping to suggest separation while allowing for barrier free access between the school and park.
2. Outdoor recreation facilities such as ball fields, ball courts or play ground areas should orient toward boundary line between park and school for easy access from both land uses.
3. Methods of securing the school building should not restrict the after hours use of the schools playgrounds, ball fields or play courts.

4. Should direct physical separation of the school and park site be desired, open construction fences should be employed to allow for unobstructed views into both the school and park. Vinyl coated chain link fences may be allowed as view fences between schools and parks, but openings or gates should be provided within open construction fences to allow for the potential for shared use.

k. Residential (PA 47)/Commercial (PA 49) Interface (Figure IV-15A)

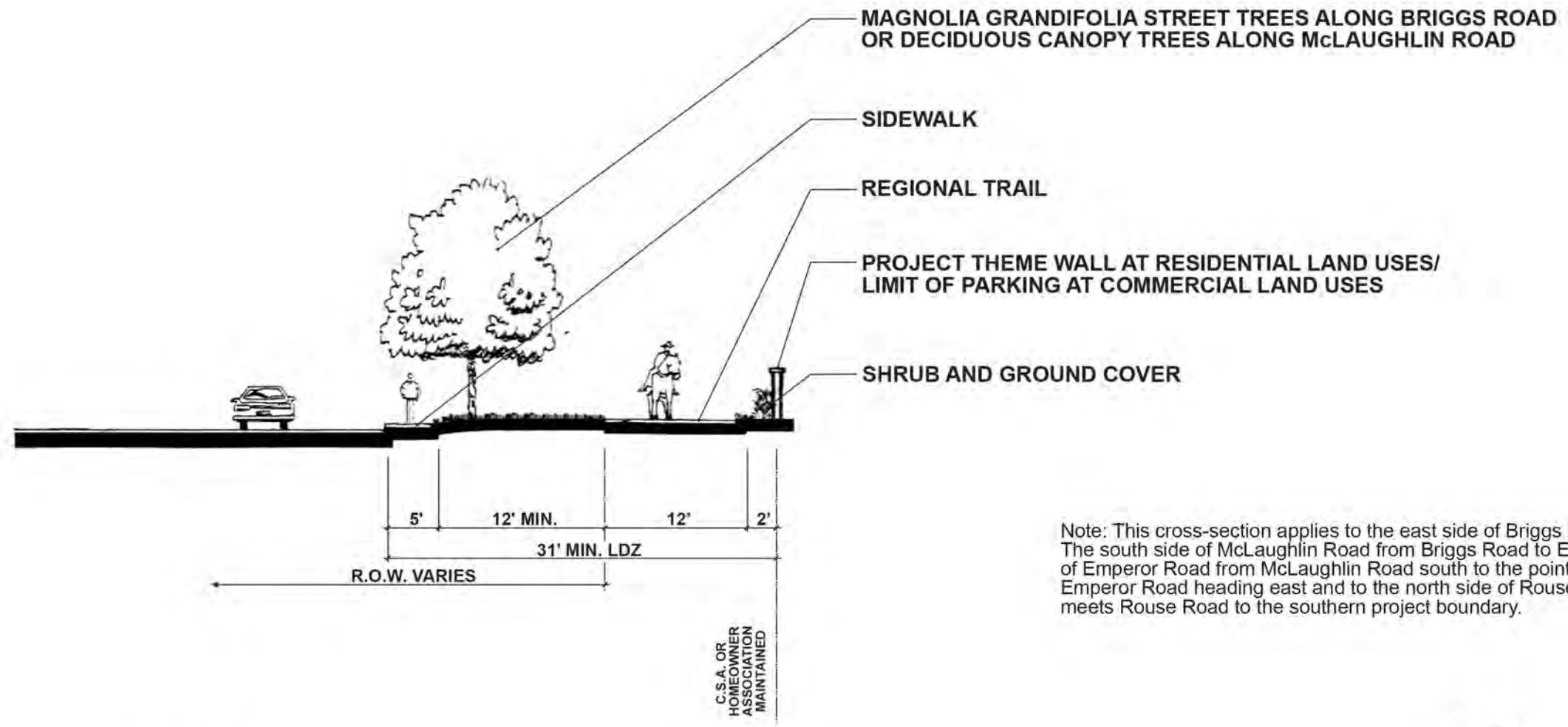
This interface, shown in Figure IV-15A occurs where residential land uses, either existing or proposed, are adjacent to Commercial Retail land uses. The following guidelines apply:

1. A 6-foot high community theme wall shall be provided adjacent to the commercial land uses. Figure IV-21 illustrates the *Community Walls and Fencing Plan for MENIFEE NORTH*.
2. A slope landscaped with trees, shrubs, and groundcovers will provide screening and buffering for the residential uses from the commercial uses.
3. A 6-foot high community theme fence shall be provided at the residential side of the landscaped buffer.

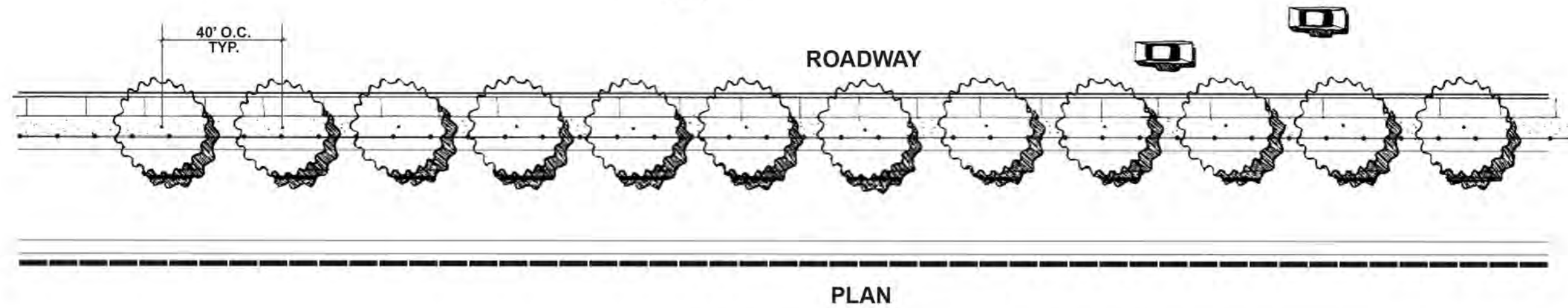
l. Residential (PA48)/Off-Site Residential (South) Interface (Figure IV-15B)

This interface is created where proposed residential land uses within MENIFEE NORTH are adjacent to existing off-site residential to the South. Figure IV-15B illustrates the on-site residential/existing adjacent residential land use transition which incorporates the following guidelines:

1. A landscaped slope is provided for on-site residential lots to create a buffer from off-site.
2. Encourage on-site residential lots to establish generous backyard landscaping with informal groupings of both vertical and canopy trees to limit views from adjacent existing residential properties to proposed residential roof lines.
3. A community theme fence shall be provided at the property line.
4. Where grading changes occur, incorporate contour grading techniques, avoiding sharp cut manufactured slopes to create a more natural appearance.



Note: This cross-section applies to the east side of Briggs Road north of McLaughlin Road. The south side of McLaughlin Road from Briggs Road to Emperor Road and the east side of Emperor Road from McLaughlin Road south to the point where the regional trail departs Emperor Road heading east and to the north side of Rouse Road from a point where the trail meets Rouse Road to the southern project boundary.



Source(s): ACD Engineering

FIGURE IV-14

Not to Scale

REGIONAL TRAIL/ROADWAY INTERFACE

IV. DESIGN GUIDELINES
 MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

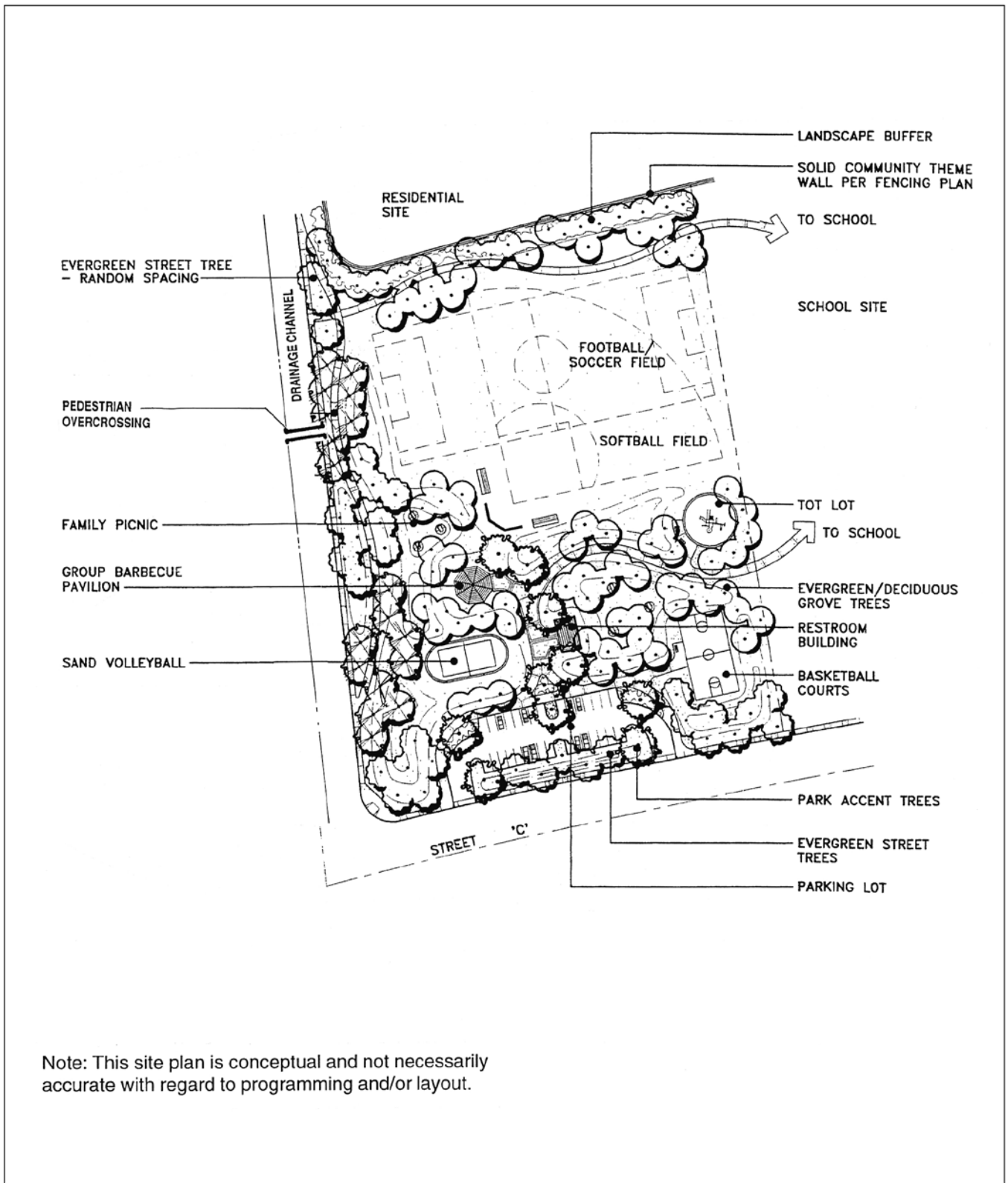
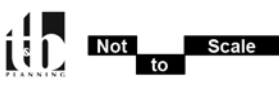


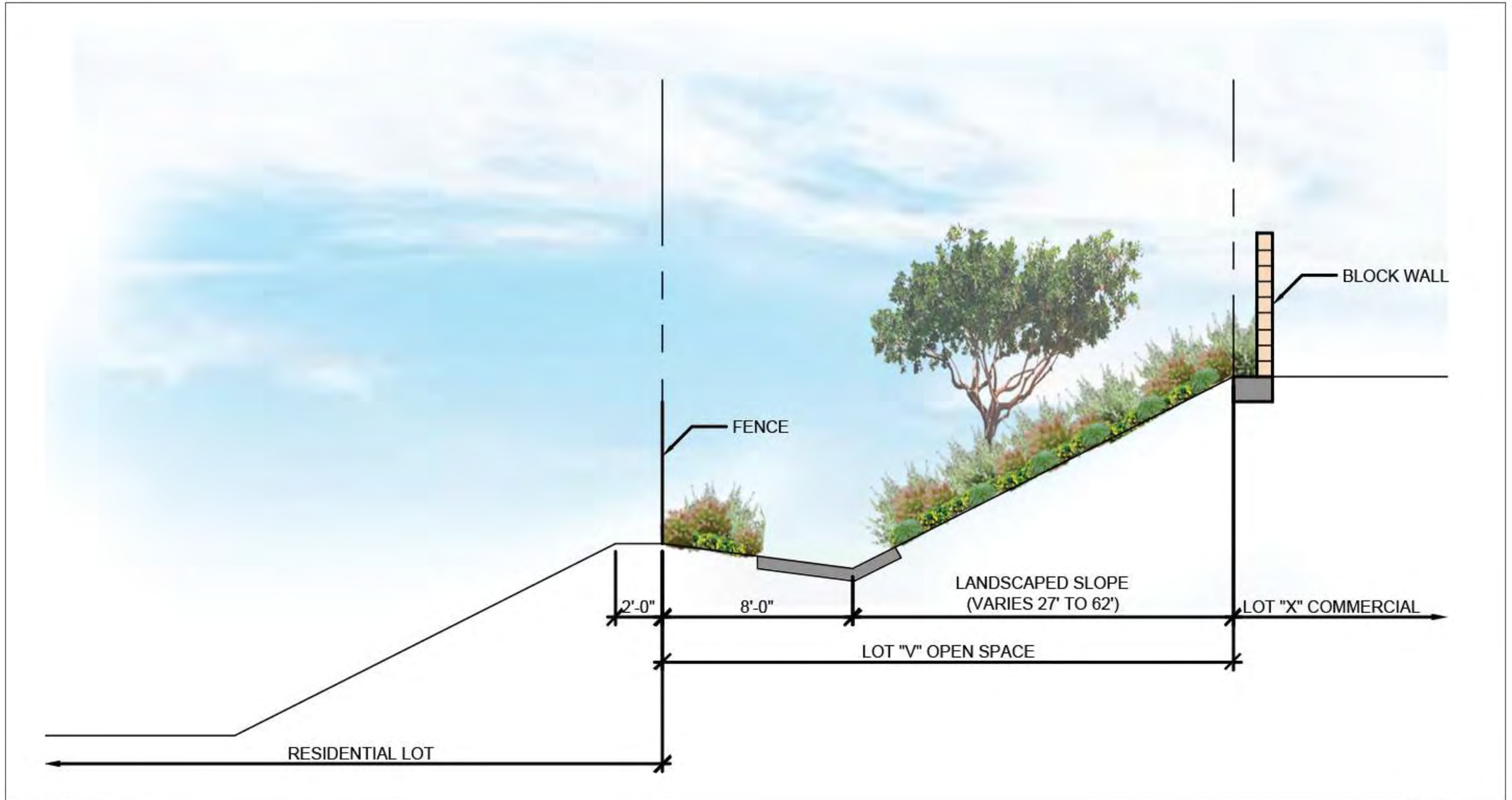
FIGURE IV-15



COMMUNITY PARK/SCHOOL SHARED USE INTERFACE


IV. DESIGN GUIDELINES

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4



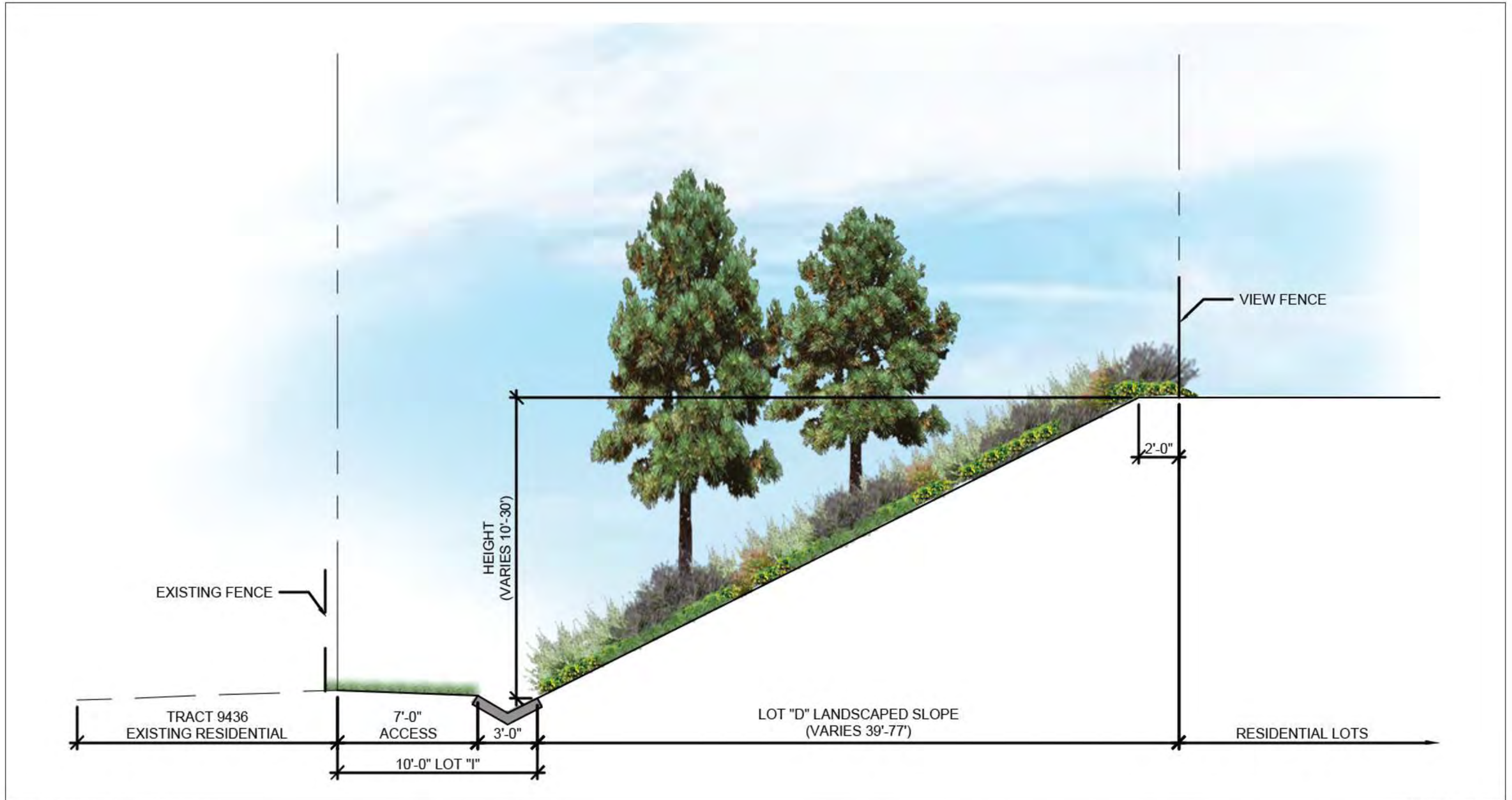
Source(s): Brightview Design Group (11-01-2021)

FIGURE IV-15A


 Not to Scale

RESIDENTIAL (PA 47)/COMMERCIAL (PA 49) INTERFACE

IV. DESIGN GUIDELINES
 MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4



Source(s): Brightview Design Group (11-01-2021)

FIGURE IV-15B

Not to Scale

RESIDENTIAL (PA 48)/OFF-SITE RESIDENTIAL (SOUTH) INTERFACE

IV. DESIGN GUIDELINES
 MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

3. STREETSCAPES

The *Conceptual Landscape Plan*, Figure IV-1, establishes the streetscape hierarchy and continuity of the *MENIFEE NORTH* project. The plant palette and landscape treatments for each of the streets serve to emphasize certain key aspects of each corridor. Plant materials will be selected for their ability to express a dynamic design nature which will be dependent upon their particular sizes, shapes, forms, textures and flowering habits. The particular physical characteristics and growth habits of streetscape plant materials will allow them to influence the identities and functions of the street system on which they are placed.

a. **Highway 74 / Ethanac Road Streetscape** (Figure IV-16)

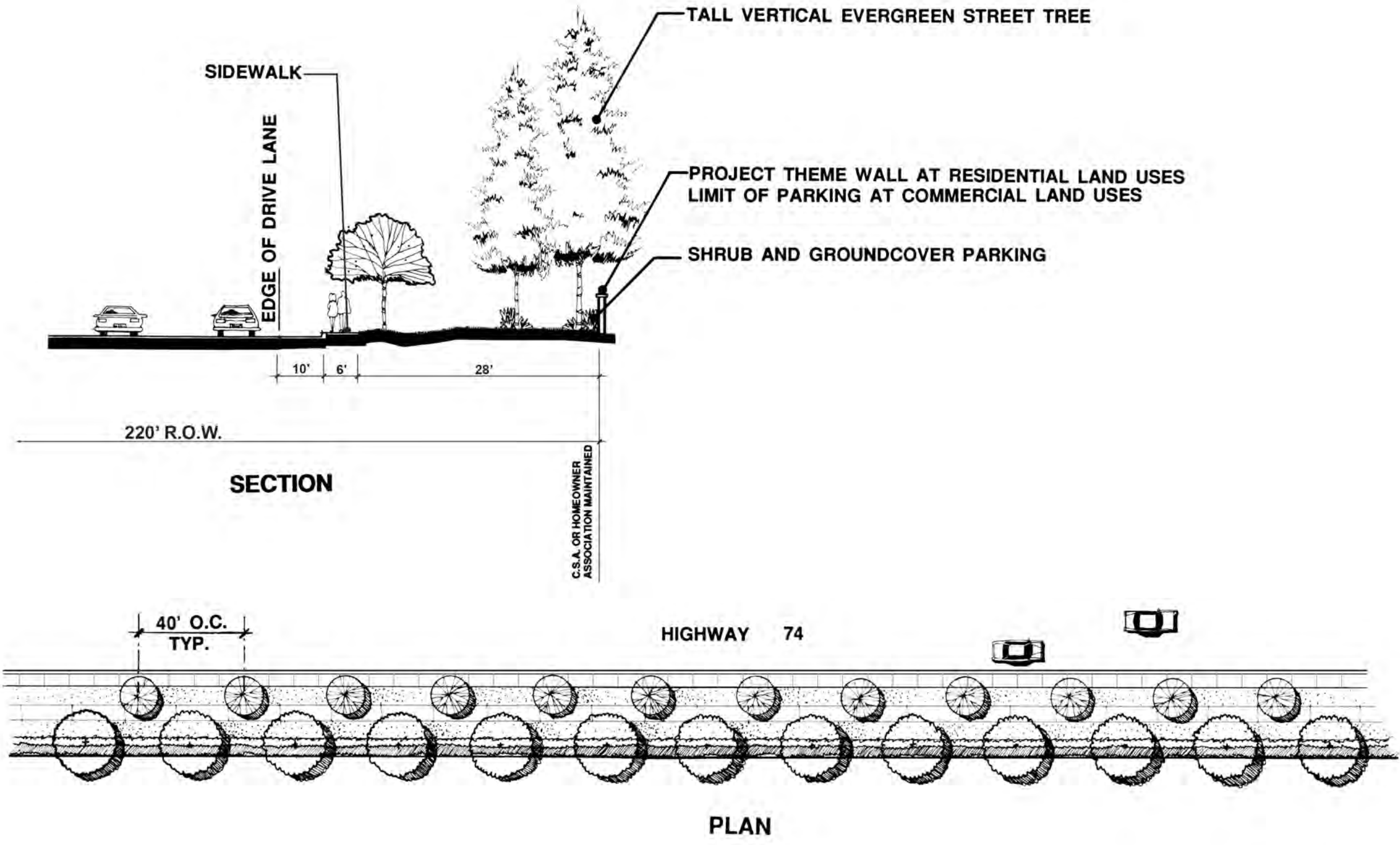
State Highway 74 and Ethanac Road (combined) is the most significant corridor of the *MENIFEE NORTH* project, forming the major entries from the east and the west. The *Specific Land Use Plan* (Figure III-1) appropriately provides for commercial uses along this established corridor, transitioning outward to more rural land uses which are compatible with the surrounding area. A formal statement of distinctive tall evergreen trees, as depicted in Figure IV-17, not only strengthens the suburban nature of this streetscape but creates a feeling of grand entry to the community of *MENIFEE NORTH*. Because of the high visibility requirement necessary for successful commercial uses, the plantings along this streetscape are to be simple and uncluttered, allowing views to penetrate commercial uses. Backdrop trees will be evergreen for year-round marking appeal, with flowering accent trees in the foreground and center median. Detailed plantings are reserved for individual uses and monumentations. Landscape berms with a formal hedge will screen parking lots and support and strengthen this streetscape.

b. **Briggs Road Streetscape** (Figure IV-17)

Because Briggs Road continues through adjacent proposed and existing communities, special consideration has been given to create continuity throughout these projects. For this reason, the Magnolia grandiflora has been selected as the street along Briggs Road. Architectural themes, wall entries, and setbacks have also been coordinated for a compatible streetscene.

c. **Typical Secondary and Industrial Collector Road Streetscape** (Figure IV-18)

To achieve a balance between the projects suburban and rural characteristics, community streetscapes will employ a combination of plant material types and planting arrangements. Generally, the remainder of the streets within the community with a higher traffic volume should express a more suburban nature as depicted in Figure IV-18.



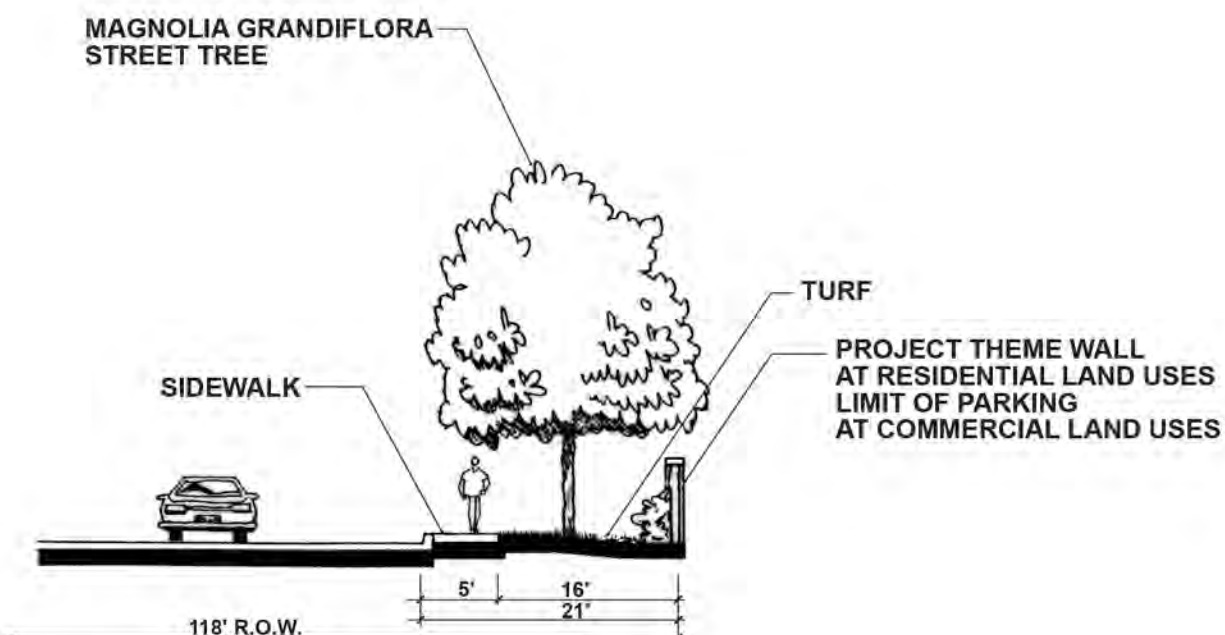
Source(s): ACD Engineering

FIGURE IV-16

Not to Scale

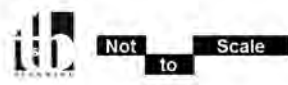
HIGHWAY 74/ETHANAC ROAD STREETSCAPE

IV. DESIGN GUIDELINES
MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4



Note: This cross-section applies to the east side of Briggs Road (south of McLaughlin)

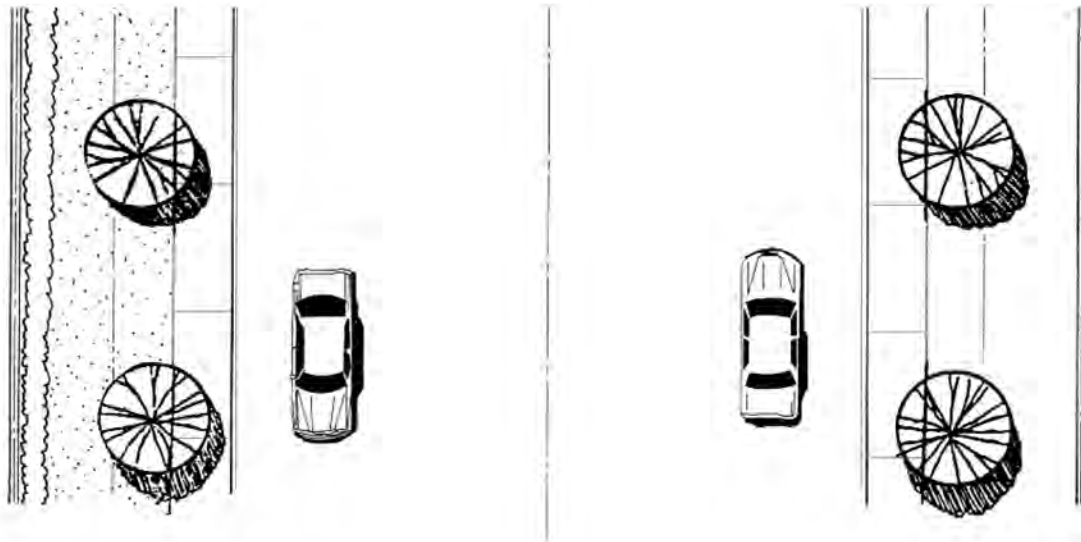
FIGURE IV-17



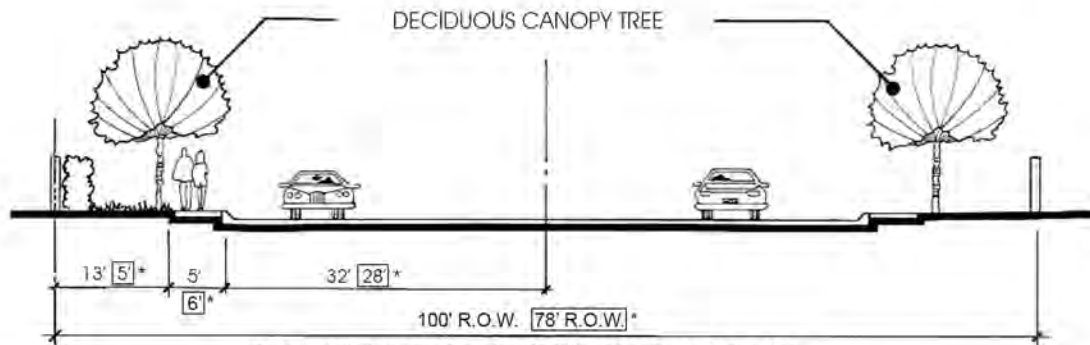
BRIGGS ROAD STREETSCAPE

IV. DESIGN GUIDELINES

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4



PLAN



*78' R.O.W. APPLIES TO INDUSTRIAL COLLECTOR ROADS

SECTION

NOTE:

SECONDARY CROSS-SECTION APPLIES TO:

- Watson Road
- McLaughlin Road (Briggs Road to Emperor Road)
- Rouse Road

*INDUSTRIAL COLLECTOR CROSS-SECTION APPLIES TO:

- *Sultanas Road (Highway 74 to North Boundary of Planning Area 44)
- *Street "B" (Briggs Road to Emperor Road)

FIGURE IV-18

TYPICAL SECONDARY AND INDUSTRIAL COLLECTOR ROAD STREETSCAPE

IV. DESIGN GUIDELINES

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4



Not to Scale

d. Norma Jean/Sultanas Road Streetscape Adjacent to Planning Areas 31A and 32 (Figure IV-19)

To limit views of Planning Areas 31A and 32 from adjacent existing residential uses to the east, a special landscape treatment will be provided along this portion of Norma Jean/Sultanas Road. Similar to secondary and industrial roadways, this streetscape will employ a combination of plant material types and planting arrangements to convey a rural atmosphere.

e. Leon Road Streetscape (North of Watson Road) (Figure IV-19A)

The 74-foot Leon Road right-of-way North of Watson Road consists of 34 feet of paving with a 17-foot buffer and 7 feet of future dedication on the west side. The east side of the road has a 7-foot landscaped parkway and 7 feet of proposed dedication including a 5-foot parkway-adjacent sidewalk and landscape buffer.

f. Leon Road Streetscape (South of Watson Road) (Figure IV-19B)

The 74-foot Leon Road right-of-way South of Watson Road consists of 34 feet of paving with a 17-foot buffer and 7 feet of future dedication on the west side. The east side of the road has a 7-foot landscaped parkway, a 5-foot parkway-adjacent sidewalk, and a landscape buffer.

g. Alicante ~~DriveRoad~~ Streetscape (Figure IV-19C)

The 74-foot Alicante ~~DriveRoad~~ right-of-way consists of 34 feet of paving with a 18-foot proposed swale and 7 feet of future dedication on the northerly side. The southerly side consists of a 7-foot landscaped parkway and 7 feet of proposed dedication including a 5-foot parkway-adjacent sidewalk and landscape buffer.

4. COMMUNITY OPEN SPACE

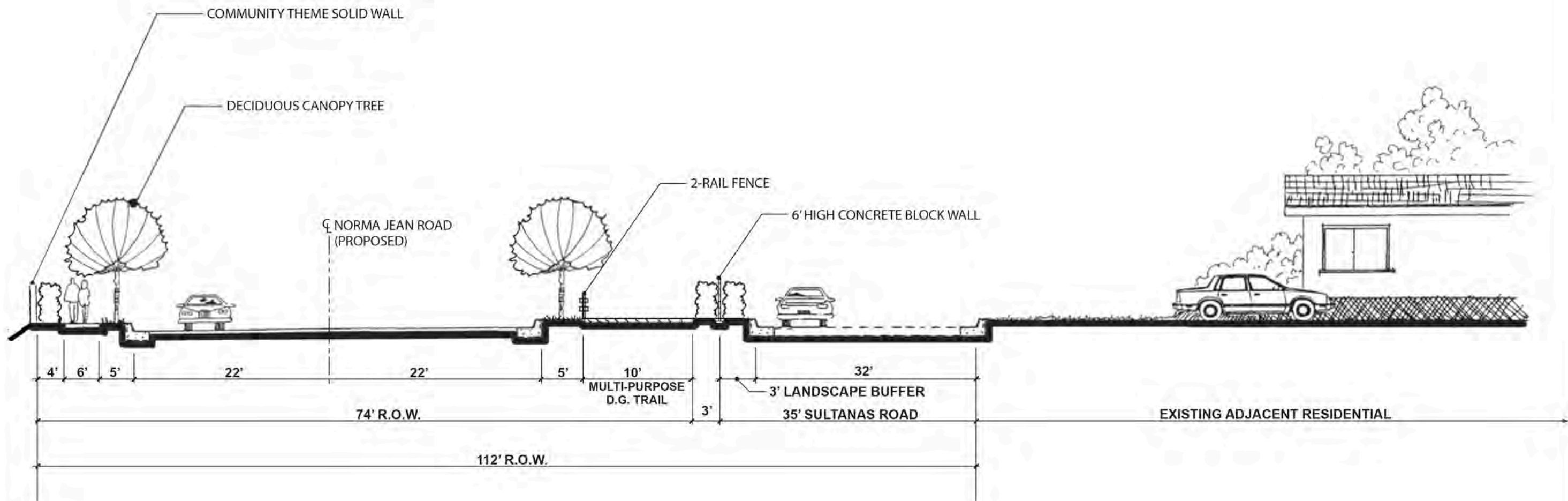
The *MENIFEE NORTH* Specific Plan offers a variety of open space and recreational opportunities as discussed below.

a. Natural Open Space Areas

Substantial amounts of natural open space will be preserved in the southeastern portion of the project area. The open space areas will be developed with a regional trail system traversing the slope at the southeastern portion of the project. Benefits in terms of significant view opportunities and physical buffering serve an important land use function. Residential land uses which are adjacent to the areas will be allowed to take advantage of the visual qualities of the open space areas due to its up-hill relationship to residential properties. Prominent visual continuity with the open space will visually extend the limits of those yards into the open space. No physical improvements or structures, with the exception of the regional trail, would be allowed to encroach into the open space areas. Hillside vegetation, however, may be selectively thinned as required to limit fire hazards. A fuel modification zone as previously described in the Residential/Regional Trail/Open Space Interface Section and has been included in this Specific Plan to address potential fire hazards within open space areas.

b. Community Center/Park/School Shared Use (Figure IV-15)

Where a "Shared Use" concept between school sites and the adjacent community parks can be developed, additional improved open space areas (play fields, game courts, etc.) within the school site can also become an integral component of the projects Community Open Space. The park and school will be developed to the specifications of the County of Riverside and the governing school district. Pursuant to existing requirements, the park will include multipurpose playing fields, multi-purpose courts, restrooms, picnic and parking facilities. The landscaping theme will be consistent with the streetscape and edge interface conditions as previously discussed. A Conceptual site plan for the 5.5-acre community park is shown in Figure IV-15.



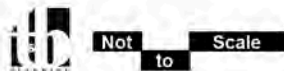
NOTE:

ALL IMPROVEMENT TO NORMA JEAN ROAD/SULTANAS ROAD SHALL BE RESPONSIBILITY OF THE DEVELOPERS OF PLANNING AREAS 31A AND 32 AT NO EXPENSE TO EXISTING RESIDENTS.

IMPROVEMENTS SHALL OCCUR WITHOUT ANY FURTHER ENCROACHMENT THAN CURRENTLY EXISTS INTO FRONT YARDS OF EXISTING RESIDENCES.

Source(s): ACD Engineering

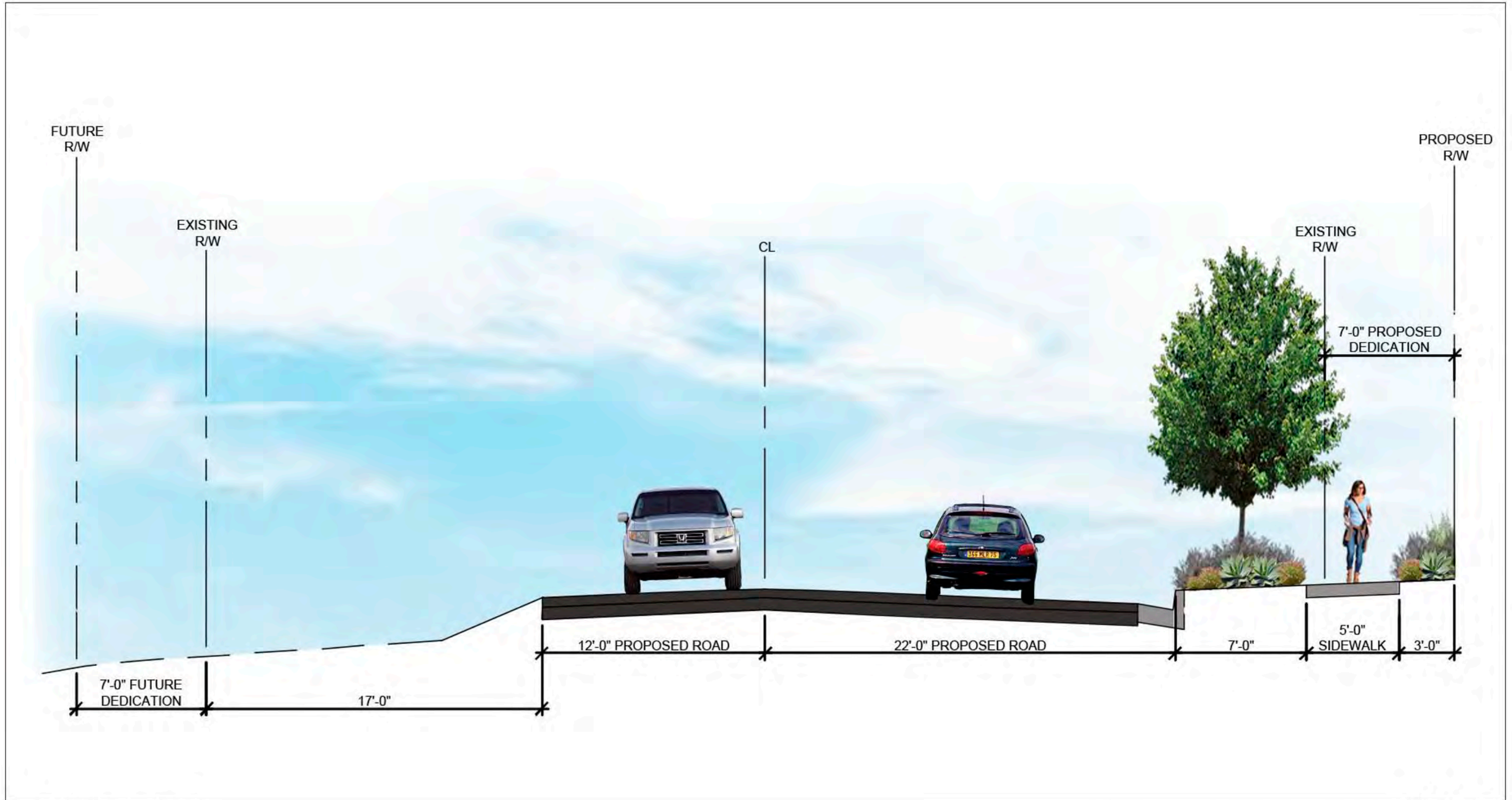
FIGURE IV-19



NORMA JEAN ROAD/SULTANAS ROAD STREETScape ADJACENT TO PLANNING AREAS 31A AND 32

IV. DESIGN GUIDELINES

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4



Source(s): Brightview Design Group (11-01-2021)

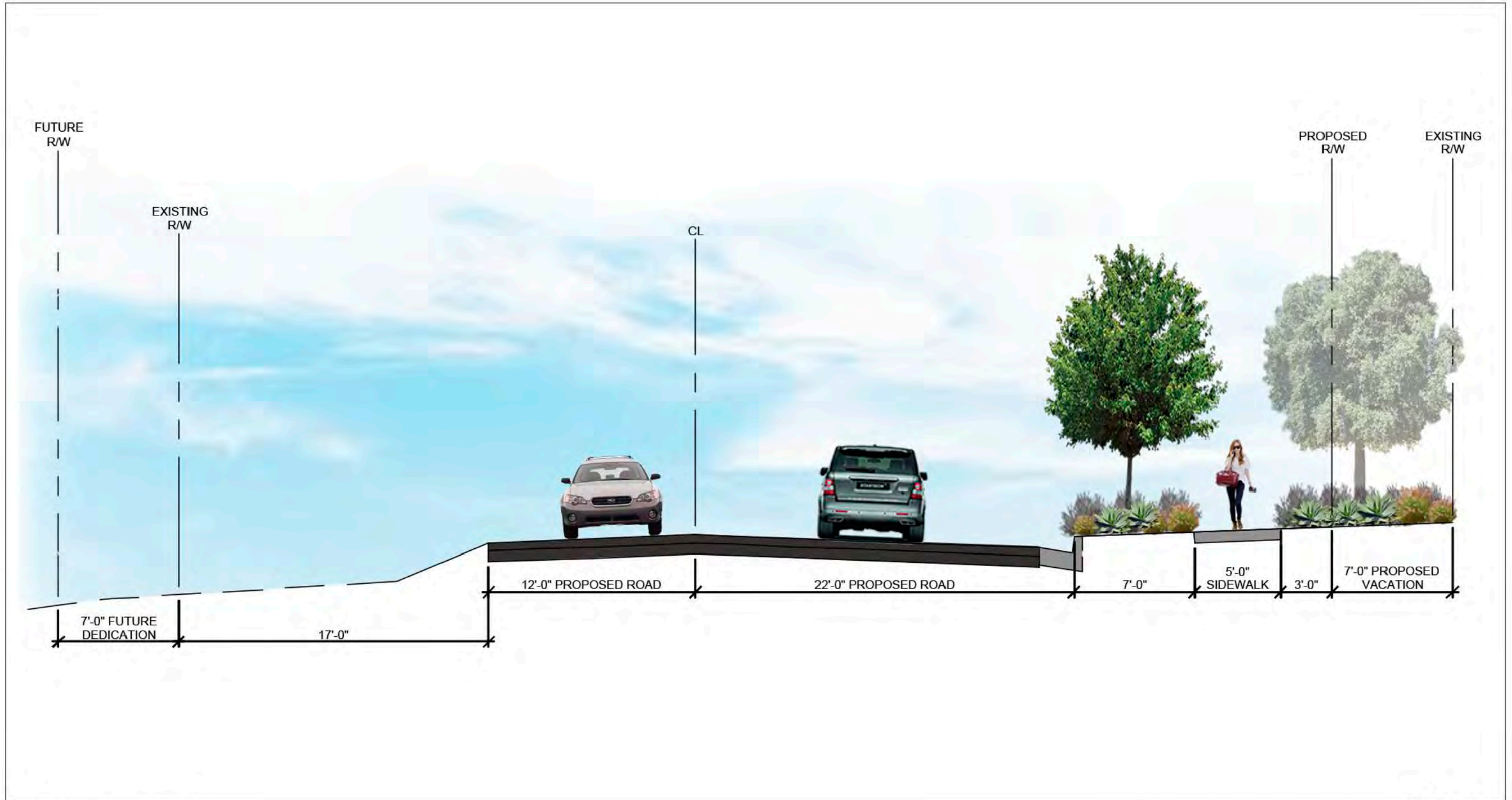
FIGURE IV-19A

Not to Scale

LEON ROAD STREETScape (NORTH OF WATSON ROAD)

IV. DESIGN GUIDELINES

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4



Source(s): Brightview Design Group (11-01-2021)

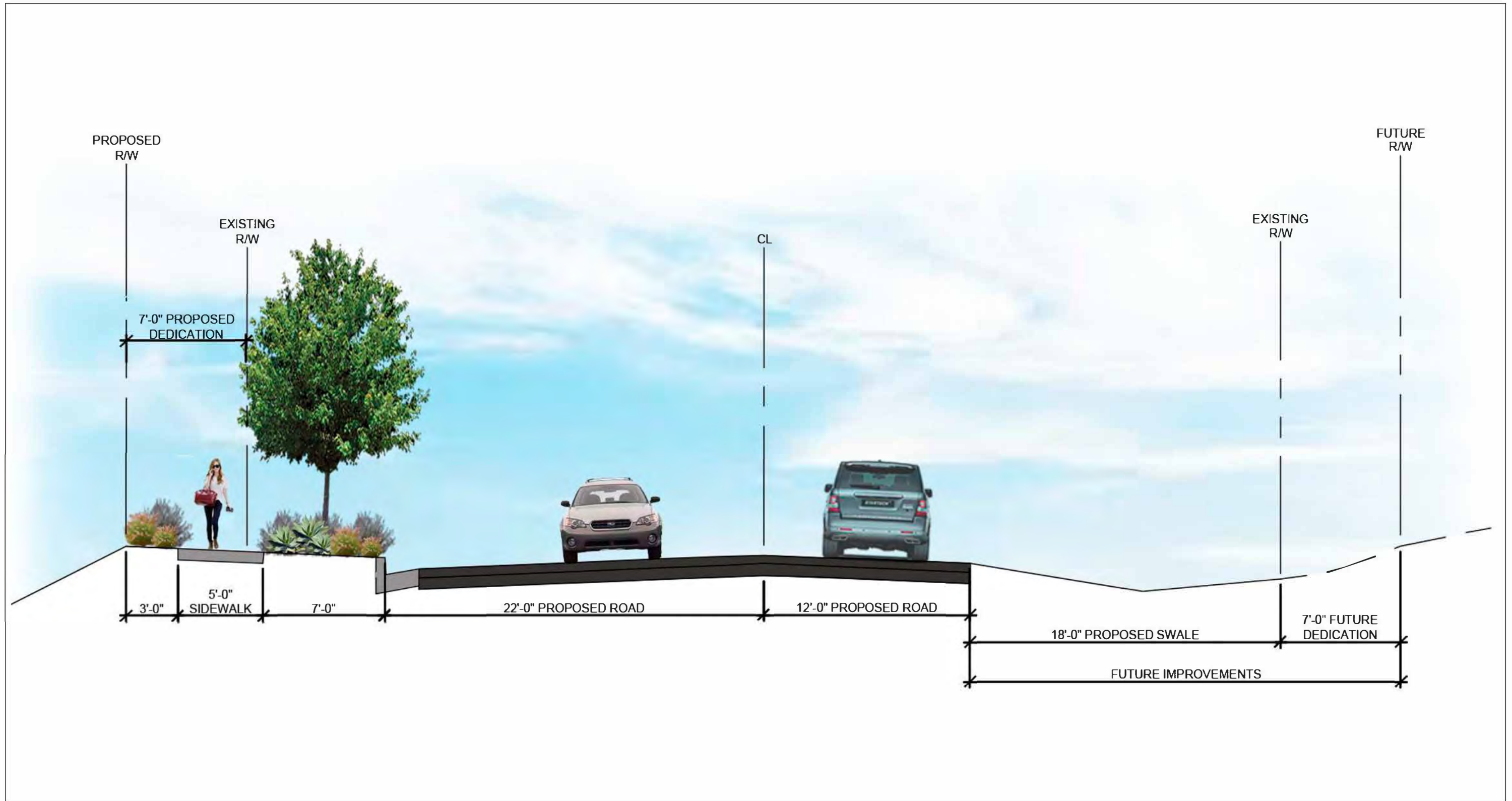
FIGURE IV-19B

th Not to Scale

LEON ROAD STREETScape (SOUTH OF WATSON ROAD)

IV. DESIGN GUIDELINES

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4



Source(s): Brightview Design Group (11-01-2021)

FIGURE IV-19C

th **Not to Scale**

ALICANTE DRIVE STREETSCAPE

IV. DESIGN GUIDELINES

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 3

X-X

c. Mini-Parks (Figure IV-20)

The land use plan proposes the development of five mini-parks located in Planning Areas 26, 32, 35, 37, and 47. These parks vary in size from 0.2 to approximately 4-5 acres and are intended serve as gathering places for the residential enclaves in which they are located. Generally these parks might be improved to include a picnic area with tables, benches and barbecues, and a turfed area or sand surfaced tot lot or playground. The park would be planted with a combination of Eucalyptus, Pine or Sycamore tree species to provide shade and to define the individual active or passive use areas. A meandering paved, pedestrian pathway or walkway system could be provided for the park and, where appropriate, would be linked to pedestrian walkway systems to adjacent residential neighborhoods.

d. Public Park

A 4.9-acre Public Park is located within Planning Area 25. Park amenities may include, but are not limited to, a sports field, basketball court, a picnic area with tables, benches and barbecues, and a turfed area or sand surfaced tot lot or playground. A meandering paved, pedestrian pathway or walkway system could be provided for the park and, where appropriate, would be linked to pedestrian walkway systems to adjacent residential neighborhoods. The landscaping theme will be consistent with the streetscape and edge interface conditions as previously discussed.

5. COMMUNITY WALLS AND FENCING

Community walls and fences that can be viewed from the public streets form an integral park of the community design. They can unify the various land uses and reinforce the design themes established at the project entries. A set of standards has been developed for community walls and fences as illustrated in Figure IV-22, *Community Walls and Fence Details*. The location of each wall or fence is conceptually illustrated in Figure IV-21, however, the location will vary depending upon final detail design and grading considerations. In general, solid walls will be located adjacent to residential uses along major highways and secondary roads, where the lot is at approximately the same grade as the street, and as screen elements between incompatible land uses. Concrete, decorative block, or stucco with brick or molded caps are all the accepted materials for these walls. In cases where side and rear yards have desirable views, in which security is a prime consideration but a solid wall is not necessary to inhibit fire or wildlife, high quality wrought iron or tubular steel-type fencing should be employed. Solid walls may be increased by up to two (2) feet with a retaining wall, where required.

Finally, a solid wood or vinyl fence, as depicted in Figure IV-22, is acceptable for use between residential lots.

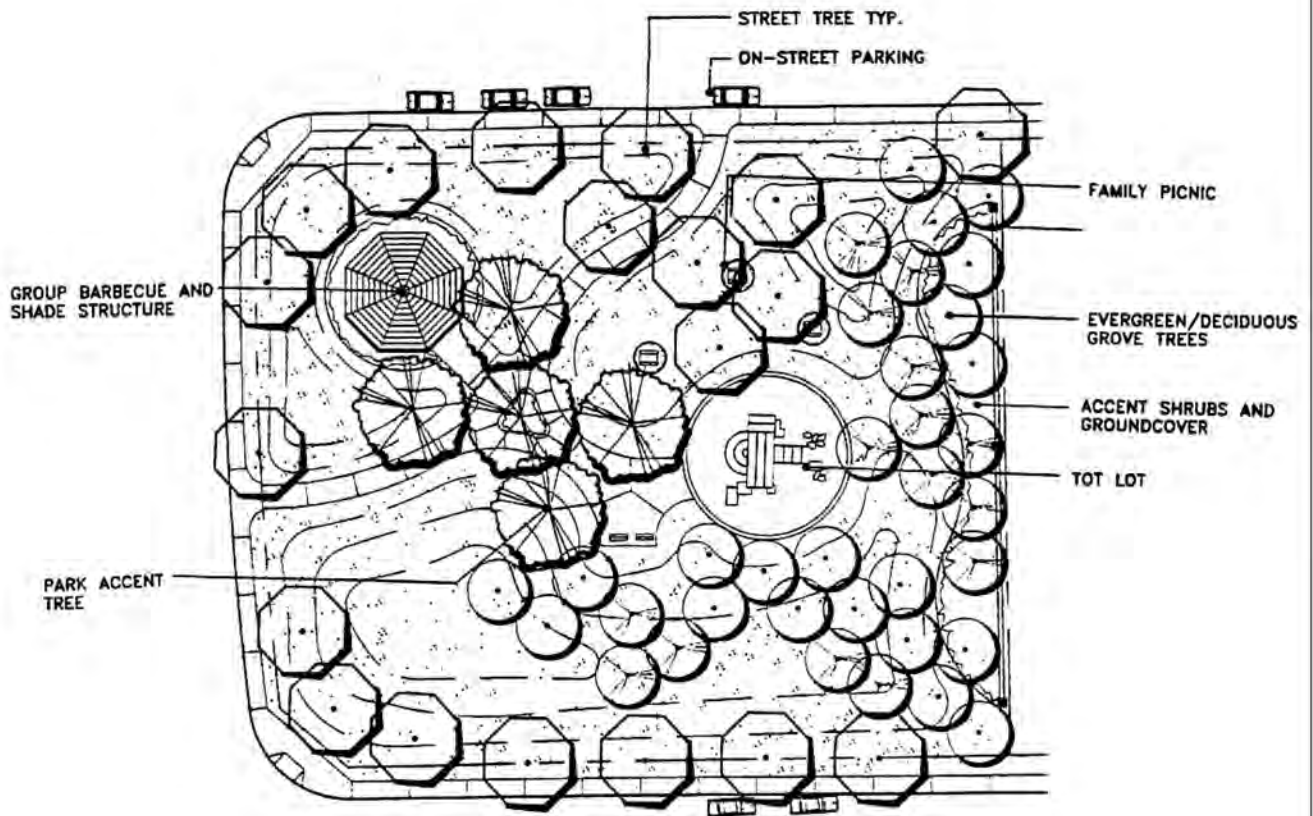
6. SIGNAGE

The *MENIFEE NORTH* Specific Plan area is intended as a high-quality, mixed-use community. Therefore, only a limited amount of signage shall be permitted in the community.

a. General Intent

- Signage shall be attractive and subdued in keeping with the character of a contemporary urban environment.

- Signs shall be limited to community identification, direction and building or company identification only. Signs should not be used for product advertising.
- All business signs shall be compatible with the architecture of the building they identify.
- Retail commercial signage shall be limited to below the roofline and be consistent in color and theme. Monument signs are encouraged and should be of a size only necessary for identification.



NOTES:

1. Mini-parks are required in Planning Areas 26, 32, 35, 37, and 47.
2. This site plan is conceptual and not necessarily accurate with regard to programming and/or layout.

FIGURE IV-20



Not to Scale

TYPICAL MINI-PARK

IV. DESIGN GUIDELINES

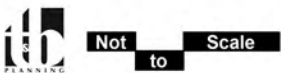
MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4



LEGEND	
1	SHADE STRUCTURE
2	5-12 TOT LOT
3	2-5 TOT LOT
4	TURF
5	BBQ
6	PICNIC AREA

Source(s): Brightview Design Group (10-14-2022))

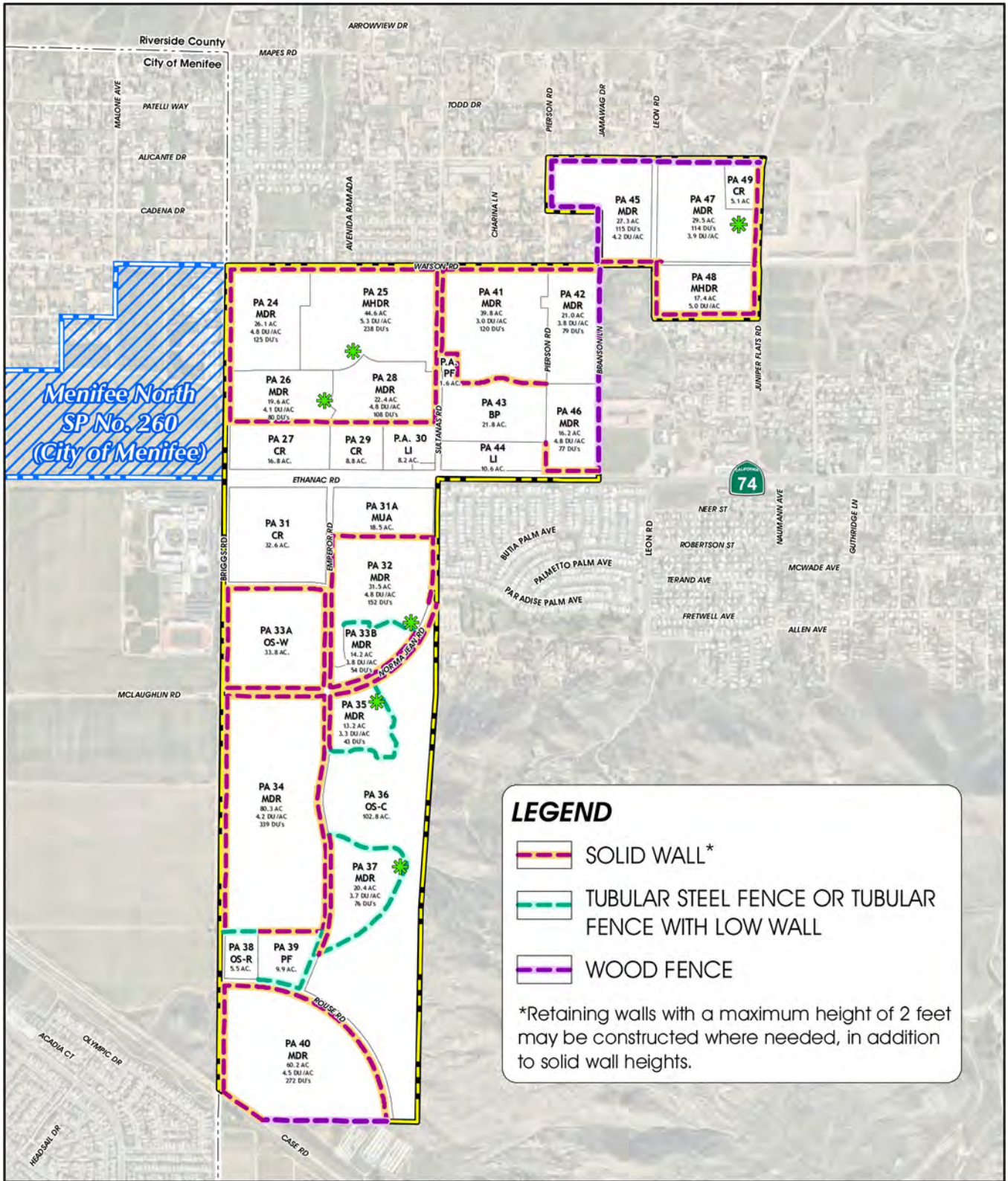
FIGURE IV-20A



CONCEPTUAL PARK PLAN (PA 47)

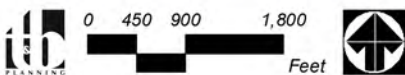
IV. DESIGN GUIDELINES

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4



Source(s): ESRI, Near Map Aerial (2021), RCTLMA (2021)

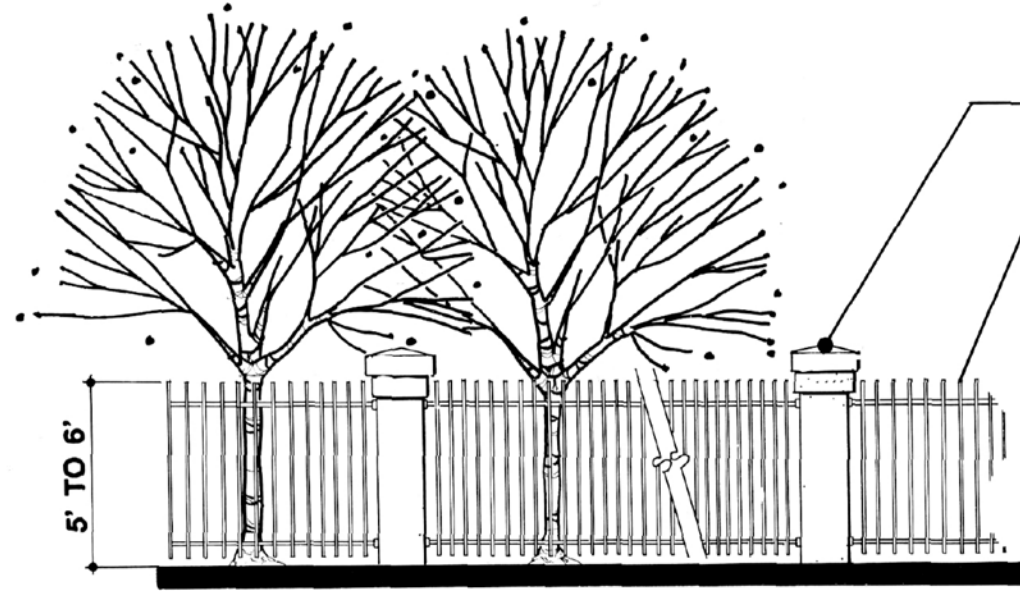
FIGURE IV-21



COMMUNITY WALLS AND FENCING PLAN

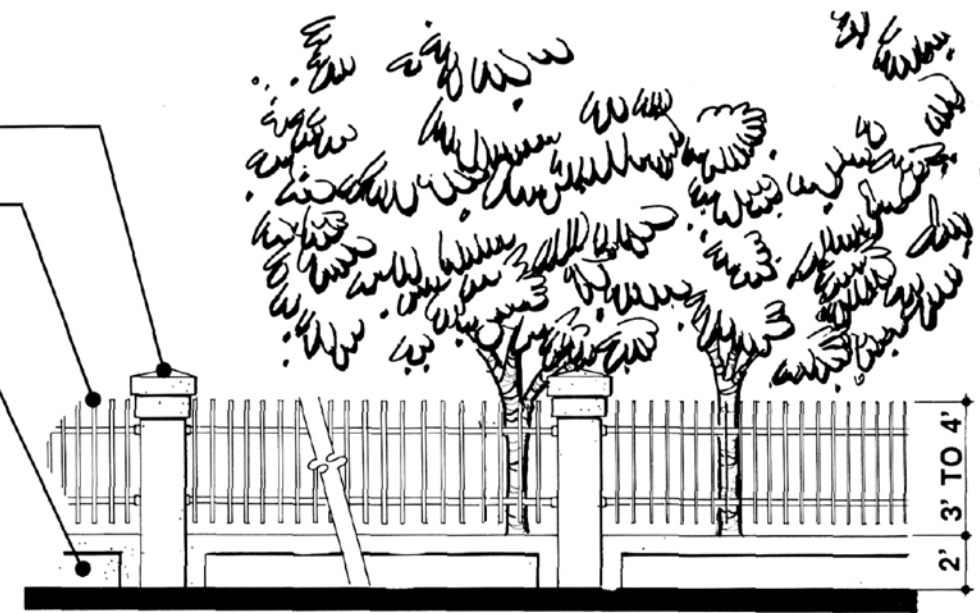
IV. DESIGN GUIDELINES

MENIFEE NORTH SPECIFIC PLAN NO. 260 AMENDMENT NO. 4

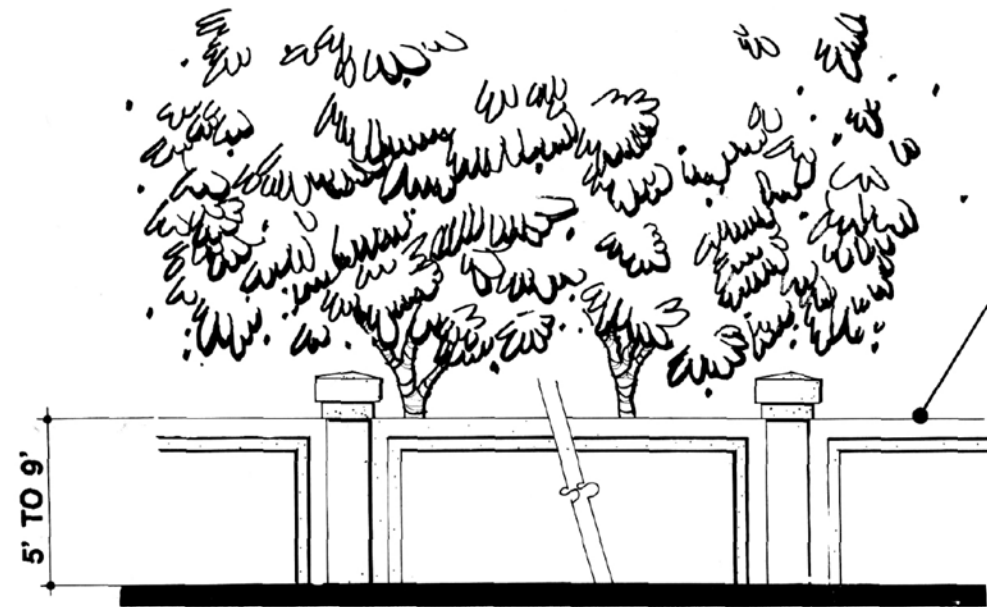


TUBULAR STEEL FENCE

STUCCO PILASTER
 TUBULAR STEEL FENCE
 LOW STUCCO WALL

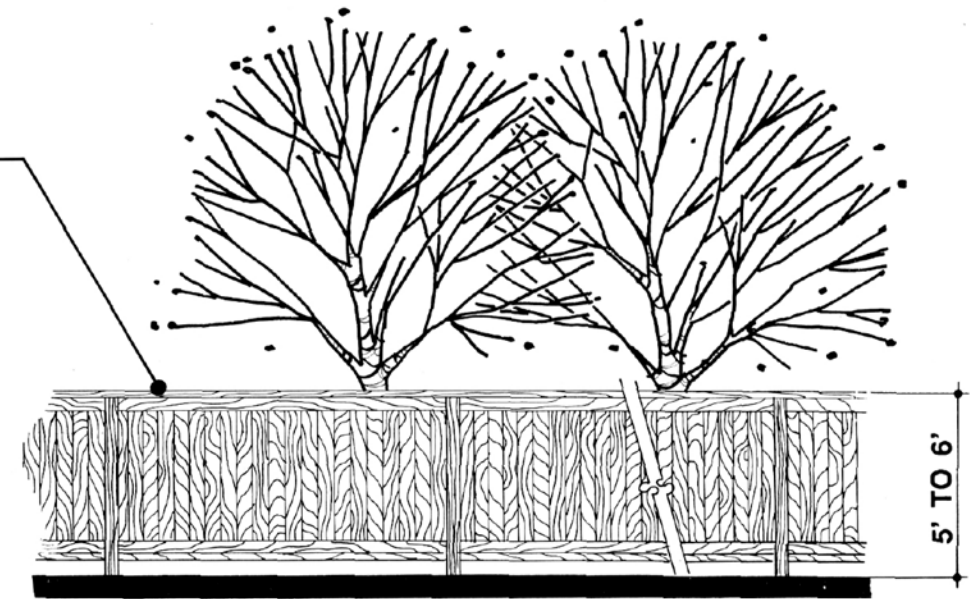


TUBULAR FENCE WITH LOW WALL



SOLID WALL*

WOOD OR VINYL FENCE
 STUCCO WALL



WOOD OR VINYL FENCE

*NOTE: HEIGHT MAY ONLY EXCEED 6' IN PA'S 32 & 33B

FIGURE IV-22

1. Residential signs should be coordinated and integrated with the neighborhoods in which they are erected.
2. All signs should be well integrated into site landscaping.
3. All signs shall be compatible with the overall theme of the Menifee North Specific Plan area and shall be in conformance with the Riverside County Zoning Ordinance Standards.
4. All signs shall be of professional quality.
5. No signs shall be painted directly on any building elevation.
6. Signage design and schematics, showing location, size, dimensions, type style and colors, shall be submitted with the required plot plan for the retail commercial site. Residential signage shall be controlled through CC&Rs or other appropriate mechanism.

b. Prohibited Signs

1. Pylon signs shall be prohibited.
2. Revolving, rotating or moving signs shall be prohibited.
3. Signs with flashing or blinking lights shall be prohibited.
4. Roof signs shall be prohibited.
5. Projecting signs shall be prohibited.
6. Totem signs displaying the names of each tenant shall be prohibited.
7. Billboards shall be prohibited.
8. Portable signs, including vehicles used as billboards, shall be prohibited.
9. Signs advertising or displaying any unlawful act, business or purpose shall be prohibited.
10. Any strings, pennants, banners, streamers, balloons, inflatable objects, flags, twirlers, propellers, noise-emitting and similar attention getting devices shall be prohibited, except:
 - 1) National, state, local governmental, institutional or corporate flags should be allowed. Flag pole locations should be shown on the site plan for approval. Only one set of flags should be allowed on each site.
 - 2) Holiday decorations in season, used for an aggregate of 45 days in any one calendar year, should be allowed.

- 3) Pennants, banners or flags used in conjunction with subdivision sales offices and tract entry points should be allowed during the initial sales period.

c. Retail Commercial Monument Signs

1. Monument signs are the preferred signage type for the Specific Plan area.
2. Monument signs should be set back a minimum of twenty feet from the face of curb (where no sidewalk is provided) and a minimum fifteen feet from the inboard edge of walk where a sidewalk or path is provided. Signs should be set back a minimum of fifteen feet from the property line and five feet from any driveway.
3. Only one monument sign shall be allowed on each site.
4. Monument signs should have a concrete or masonry base, and should be compatible with the building architecture.
5. Double sided monument signs are allowed.
6. Monument sign lighting is allowed but not internal lighting. External lighting is encouraged so long as the lighting source is well concealed and consistent with the plan area design objectives.

d. Building Mounted Signs: Retail Commercial Buildings

1. Multi-tenant retail commercial buildings within areas zoned for commercial land use should have only one sign for each tenant.
2. Tenant signage should be well integrated into the building design.
3. Multi-tenant retail commercial facilities shall have a cohesive, well integrated signage program. The combined impact and interrelationship of all tenant signage shall be considered in signage program design, review and approval.
4. All multi-tenant signs should be constructed of the same materials, have similar dimensions and coordinated colors.
5. Can signs shall be prohibited.
6. Internally illuminated channel letter signs are acceptable, but external, concealed and integrated lighting is preferred.
7. As noted above, externally illuminated signage may be allowed when the lighting source is well concealed and signage design is consistent with plan area design criteria.
8. The mounting of signs on projecting architectural elements shall be allowed when they are compatible with the building architecture and compatible with the plan area design objectives.

e. Multi-Tenant Building Signs

1. Multi-tenant signage shall be coordinated with the building architecture occurring in a pattern on the building elevation.
2. Multi-tenant signage should be subdued and more restrained than commercial retail signage. All multi-tenant signs should be constructed of the same materials, have similar dimensions and coordinated colors.
3. Signs should be similar in design having the same size, shape and color.

f. Community Entry Monumentation

1. Signage for community entry monumentation is highly encouraged.
2. Design of such monumentation shall be coordinated with the street landscape and street furniture. Monumentation should inform the viewer, through written and symbolic graphics, that the planned community or development is being entered.
3. Monuments should be compatible with the design theme of the *MENIFEE NORTH* Specific Plan and the developments or communities they identify.
4. Entry monuments should establish the community or development design theme. Logo, logotype and color schemes shall be consistent throughout the area being identified.
5. Entry monuments may be illuminated externally if they are consistent with the *MENIFEE NORTH* Specific Plan design theme; glare is controlled, and lighting sources concealed.
6. Permanent, low maintenance materials, such as stone, brick, other masonry or stucco should be used.

g. Directional Signage

1. Directional signage, to identify and direct vehicular and pedestrian traffic to various on-site destinations, shall be allowed.
2. Directional signage should be of consistent design throughout the project. Signage should be fabricated from the same materials, with a consistent color palette and a common graphic theme.
3. Directional signs shall be small scale and of consistent dimensions and mounting height.
4. Directional signage should be compatible with the architectural design of the project and with the design theme of the plan area.

h. Temporary Signage

1. Signage identifying uses or activities temporary in nature, such as real estate sales and leasing, subdivision signage, contractors, design firms and developer signage associated with development or construction of buildings are allowed if consistent with the plan area design character.
2. Temporary signage shall be located on or in close proximity to uses identified.
3. Concise standards for temporary signage, including dimensional requirements, materials of construction, mounting heights, colors and graphic format should be prepared as part of the implementation of the Specific Plan, such as through CC&Rs.
4. The multitude of signs associated with development, design, construction and leasing should be combined into one design for each project.

7. LIGHTING

The level of on-site lighting as well as lighting fixtures, shall comply with any and all applicable requirements and policies of the County of Riverside. Energy conservation, safety and security should be emphasized when designing lighting systems.

1. It is recommended that all primary streets be adequately illuminated to provide for the safety and comfort of vehicular and pedestrian movement. Appropriate lighting will encourage night time use of community facilities.
2. Landscape lighting may be used for accentuating the following conditions: shrub masses, focal elements, and trees (up-lights) if properly camouflaged from view and placed at ground level without attaching to plant materials.
3. All lighting shall be designed and located in a manner which is compatible with scenic values and other public interests throughout the community.
4. General lighting shall not cast any glare onto adjacent lots and streets in such a manner as to decrease the ambience of adjacent areas or the safety of pedestrian and vehicular movement.
5. Indirect wall lighting and "wall washing" overhead down lighted or interior illumination which spills outside is encouraged.
6. Pedestrian lighting shall provide area illumination for entry ways, courtyards and other such areas.
7. Lighting fixtures shall be complimentary to the architectural concepts.

D. LANDSCAPE ARCHITECTURAL GUIDELINES AND STANDARDS

1. INTRODUCTION

The guidelines presented herein are not intended to discourage creative design or individuality. Rather, they are intended to assist in providing the continuity and desired image which will make *MENIFEE NORTH* a unique and special community.

2. MENIFEE NORTH PLANT MATERIAL PALETTE

It is the intent of the Design Guidelines to allow flexibility and diversity in planting design while defining an acceptable palette which reinforces the thematic identity of *MENIFEE NORTH*.

A limited selection of plant materials used in simple significant compositions is encouraged. Planting designs should be compatible with and complement adjacent plantings and should reinforce and enhance the individual architecture and design of each site. The materials on the plant lists have been selected for their contribution to the project theme, their adaptability to climate and soil conditions and with a concern for long-term cost effective maintenance. This plant palette requires the use of drought-tolerant materials.

MENIFEE NORTH Plant List

The plant palette for *MENIFEE NORTH* is proposed to be comprised of the following:

TREES - EVERGREEN

<u>Botanical Name</u>	<u>Common Name</u>
Acacia baileyana	Bailey Acacia
Arbutus menziesii	Madrone
Brachychiton populneus	Bottle Tree
Casuarina stricta	Mountain She-Oak
Cedrus deodara	Deodar Cedar
Cinnamomum camphora	Camphor Tree
Eriobotrya deflexa	Bronze Loquat
Eucalyptus camaldulensis	Red Gum
Eucalyptus leucoxylon 'Rosea'	White Ironbark
Eucalyptus rudis	Desert Gum
Eucalyptus sideroxylon	Red Ironbark
Ficus rubiginosa	Rustyleaf Fig
Melaleuca linarifolia	Flax Leaf Paperbark
Nerium oleander	Oleander Standard
Olea europaea 'Fruitless'	Fruitless Olive
Pinus edulis	Pinon Pine
Pinus halepensis	Aleppo Pine
Pinus pinea	Italian Stone Pine
Podocarpus gracilior	Fern Pine

TREES - EVERGREEN (continued)

<u>Botanical Name</u>	<u>Common Name</u>
Quercus agrifolia	Coast Live Oak
Quercus engelmannii	Mesa Oak
Quercus ilex	Holly Oak
Rhus lancea	African Sumac

TREES - DECIDUOUS

<u>Botanical Name</u>	<u>Common Name</u>
Albizia julibrissin	Silk Tree
Alnus rhombifolia	White Alder
Bauhinia variegata	Purple Orchid Tree
Brachychiton acerifolius	Australian Flame Tree
Fraxinus uhdeli	Evergreen Ash
Fraxinus velutina 'Modesto'	Modesto Ash
Ginkgo Biloba	Maidenhair Tree
Gledisia triacanthos	Honey locust
Koelreuteria bipinnata	Chinese Flame Tree
Langerstoemia indica	Crape Myrtle
Liquidambar styraciflua	American Sweet Gum
Liriodendron tulipifera	Tulip Tree
Pistacia chinensis	Chinese Pistache
Plantanus occidentalis	London Plane Tree
Platanus racemos	California Sycamore
Populus fremontii	Fremont Cottonwood
Populus nigra 'Italica'	Lombardy Poplar
Prunus cerasifera 'Thundercloud'	Purple Leaf Plum
Pyrus calleryana 'Aristocrat'	Ornamental Pear
Pyrus calleryana 'Bradford'	Ornamental Pear
Pyrus kawakamii	Evergreen Pear
Quercus coccinea	Scarlet Oak
Robina ambigua idahoensis	Idaho Locust
Sophora japonica	Japanese Pagoda Tree

SHRUBS (LARGE TO MEDIUM)

<u>Botanical Name</u>	<u>Common Name</u>
Abelia 'Edward Goucher'	Edward Goucher Abelia
Acacia species	No Common Name
Ceanothus species	California Lilac
Cercis occidentalis	Western Redbud
Chaenomeles species	Flowering Quince
Cistus species	Rockrose

SHRUBS (LARGE TO MEDIUM) (continued)

Botanical Name

Cotoneaster species
Elaeagnus pungens
Eriogonum giganteum
Escallonia fradesil
Euonymus japonica
Fatsia japonica
Feijoa sellowiana
Forsythia intermedia
Ilex species
Juniperus species
Leptospermum scoparium
Ligustrum japonicum
Magnolia soulangiana
Mahonia aquifolium
Nandina domestica
Nerium oleander
Osmanthus fragrans
Phormium tenax
Pittosporum tobira
Plumbago auriculata
Podocarpus macrophyllus
Prunus caroliniana
Pyracantha species
Raphiolepis species
Rhus ovata
Ribes sanguineum
Tecomaria capensis
Viburnum tinus 'Spring Bouquet'
Xylosma congestum

Common Name

Cotoneaster
SilverBerry
St. Catherine's Lace
No Common Name
Evergreen Euonymus
Japanese Aralia
Pineapple Guava
Forsythia
Holly
Juniper
New Zealand Tea Tree
Japanese Privet
Saucer Magnolia
Oregon Grape
Heavenly Bamboo
Oleander
Sweet Olive
Flez
Tobira
Cape Plumbago
Yew Pine
Carolina Laurel Cherry
Firethorn
Indian Hawthorn
Sugar Bush
Pink Winter Currant
Cape Honeysuckle
Laurustinus
Xylosma

SHRUBS (SMALL)

Botanical Name

Agapanthus africanus
Ceanothus species
Clivia miniata
Hemerocallis species
Juniperus species
Lavandula angustifolia
Liriope gigantea
Lonicera japonica 'Halliana'
Diets Vegeta

Common Name

Lily of the Nile
California Lilac
Clivia
DayLily
Juniper
English Lavender
Lily Turf
Hall's Honeysuckle
Fortnight Lily

SHRUBS (SMALL) (continued)

<u>Botanical Name</u>	<u>Common Name</u>
Mahonia aquifolium 'Compacta'	Compact Oregon Grape
Nandina domestica 'Compacta'	Compact Heavenly Bamboo
Nandina domestica 'Nana Compacta'	Dwarf Heavenly Bamboo
Nerium oleander 'Petite Pink'	Dwarf Oleander
Pittosporum tobira 'Wheeler's Dwarf'	Wheeler's Dwarf
Ribes viburnifolium	Catalina Perfume
Tulbaghia violacea	Society Garlic
Xylosma congestum 'Compacta'	Compact Xylosma

VINES

<u>Botanical Name</u>	<u>Common Name</u>
Cissus Anartica	Kangaroo Treebine
Clytostoma callistegioides	Violet Trumpet Vine
Macfadyena unguis-cati	Cat's Claw Vine
Gelsemium sempervirens	Carolina Jessamine
Jasminum mesnyi	Primrose Jasmine
Lonicera japonica	Japanese Honeysuckle
Parthenocissus tricuspidata	Boston Ivy
Wisteria floribunda	Japanese Wisteria

GROUNDCOVERS

<u>Botanical Name</u>	<u>Common Name</u>
Ajuga reptans	Carpet Bugle
Baccharis pilularis 'Twin Peaks'	Coyote Bush
Campanula poscharskyana	Serbian Bellflower
Duchesnea indica	Indian Mock Strawberry
Gazania splendens 'Mitsuwa Yellow'	Gazania
Hedera helix	English Ivy
Hypericum calycinum	Aaron's Beard
Juniperus species	Juniper
Lonicera japonica 'Halliana'	Hall's Honeysuckle
Myoporum parvifolium	Myoporum
Pelargonium peltatum	Ivy Geranium
Potentilla species	Cinquefoil
Vinca minor	Dwarf Periwinkle

3. PLANTING TIME

The *MENIFEE NORTH* area experiences temperature extremes which can make it difficult for the installation of plant materials during the hot summer months (July - September) and the cold winter months (December - March). Container plants which have not been acclimated to the region may experience heat or frost damage resulting in partial or total loss of foliage, even if these materials will be perfectly suited to the

temperature extremes once they are established. If construction schedules permit, the ideal planting time is in the spring or fall months.

4. LANDSCAPE INSTALLATION REQUIREMENTS

All areas required to be landscaped shall be planted with trees, shrubs, groundcover, vines or turf selected from the plant palette contained in these Design Guidelines.

Parcel developers should assess any existing landscaping adjacent to their property and whenever possible, reinforce and complement that established character. Detailed landscape plans are to be prepared by a licensed landscape architect for each individual project. Parcel Developers must submit landscape plans to proper agencies for approval prior to installation.

5. CLIMATIC CONSTRAINTS

- a. The plant materials for *MENIFEE NORTH* have been chosen for their ability to thrive within the site's exacting climate. The plants should grow to their full potential with a minimum amount of maintenance and replacement costs.

Precipitation, temperature, and wind are the limiting climatic factors affecting plant choice.

- b. Average annual rainfall in the area varies from nine to thirteen inches.
- c. The *MENIFEE NORTH* area's extreme temperatures range from 18 degrees in the winter to 115 degrees in the summer. The average daily temperature range is 40 to 65 degrees in the winter and 58 to 90 degrees in the summer.

6. HORTICULTURAL SOILS TEXT REQUIREMENTS

Due to the many varieties of soils within *MENIFEE NORTH*, all Parcel Developers shall be required to prepare a horticultural soils report to determine appropriate planting and maintenance requirements for specified plant materials. This soils report shall be prepared by a qualified agricultural laboratory supervised by a member of the American Soils Testing Laboratory and shall include a soils fertility and agricultural suitability analysis with pre-planting and post-planting recommendations to be included in the landscape working drawings.

7. IRRIGATION

- a. All areas requiring landscaping shall include the installation of a permanent automatic irrigation system to ensure proper plant growth. The irrigation system shall be designed to separate turf areas from shrub areas so as not to irrigate shrubs, groundcover, and lawn simultaneously.
- b. Pop-up sprinklers adjacent to all walkways and areas of pedestrian or vehicular traffic shall be spring retractable types.
- c. Pop-up sprinklers shall have matched precipitation rates.
- d. All valves shall be wired independently.

- e. Irrigation backflow prevention devices and controllers shall be located and/or screened with appropriate plant material to minimize visibility.
- f. Detailed irrigation plans are to be prepared by a licensed landscape architect.
- g. Parcel Developers must submit irrigation plans to appropriate reviewing agencies for approval prior to installation.
- h. Irrigation systems must conform to all County of Riverside requirements and State Model Water Efficient Landscape Ordinance AB1881.

8. LANDSCAPE MAINTENANCE

- a. The overall aesthetic effect of the landscape shall be an evergreen thriving plant community. Each owner shall provide continuous maintenance for all planted and hardscape areas within his/her site, keeping it free and clear of weeds, debris, rubbish, and in a neat and clean condition.
- b. All owners will be required to maintain plant materials in a thriving condition of growth by practicing proper agriculture techniques of pruning, pest control and fertilization. All palms shall be skinned periodically as necessary. Special attention must be given to preventing the installation of rootbound Eucalyptus species.
- c. From the completion of installation, landscaping shall be maintained in a sightly and well kept condition.
- d. Until maintenance responsibility is transferred to a Maintenance Organization, the Master Developer shall repair and maintain all landscaped parkways and medians. This maintenance shall include the replacement of landscaping, pavement, and lighting (if necessary), keeping the area free and clear of weeds, debris and rubbish, and in a well kept condition.
- e. No owner shall interface with the drainage of water from any site except in accordance with plans approved by all public agencies having jurisdiction.
- f. Landscaping and irrigation installed by the Master Developer or the Parcel Developer shall be maintained by them, in a healthy and operational condition until its transfer to public responsibility. All plant material failure shall be replaced with same materials that will match the size and height of adjacent material. Damaged or malfunctioning irrigation must be repaired or replaced to match the original condition of the system, and irrigation overspray and excessive runoff shall be kept to a minimum.

E. ARCHITECTURAL GUIDELINES

The architectural theme of *MENIFEE NORTH* is one of "oasis" and "comfortable shelter". The desire to reinforce the impression of coolness and a relaxed, country club atmosphere has led to a series of architectural ideas which have been deemed appropriate for dealing with the environment. The weather at *MENIFEE NORTH* is a result of a basically arid climate located in Riverside County. The summers are dry and hot, with temperatures frequently reaching 95± degrees. The high land elevation and dry climate cause a strong temperature swing downward once the sun sets, cooling the land masses and buildings. The winters can offer brisk winds and temperatures, making the warmth of a cozy hearth all that much more desirable. The buildings at *MENIFEE NORTH*, both residential and non-residential, will have to be flexible enough to cope with both the need to be cool, breezy, and open in the summer, and solid, warm, and stout in the winter.

Instead of transplanting a specific style to *MENIFEE NORTH* and attempting to define the community theme as a series of clichés, we have chosen instead to define the architecture as "appropriate". The word "appropriate" is further codified by a series of desirable details. The incorporation of these details into the fabric of each individual community will allow these communities to form their own identities and yet let them be recognized as part of the larger community.

The design elements for *MENIFEE NORTH* that will help form its distinctive character are:

- Simple, natural appearing materials.
- Soft, warm colors.
- Horizontal massing emphasis.
- Integration of indoors and outdoors.
- Cool, protected entries.
- Private patios, protected courtyards.
- Roofs with low pitches, wide overhangs.
- Windows which are grouped and shaded.
- Entry doors with a solid appearance.
- Garage doors with setbacks and appropriate solidity.
- Summer porches and verandas.
- Covered, shaded balconies and decks.
- Loggias for shade and rain protection.
- Chimneys of stone, brick, or stucco to express their mass.
- Gutters and downspouts incorporated into the design, not added on.

Design elements to avoid at *MENIFEE NORTH* are:

- Dark, moody colors or bright, flashy colors.
- Box-like architecture without detail or articulation.
- High pitched (steep) roofs.
- Silver window and door frames.
- Monotonous garage door elevations.
- Vertical massing.
- Flat, unarticulated entries.
- Exposed metal chimneys.

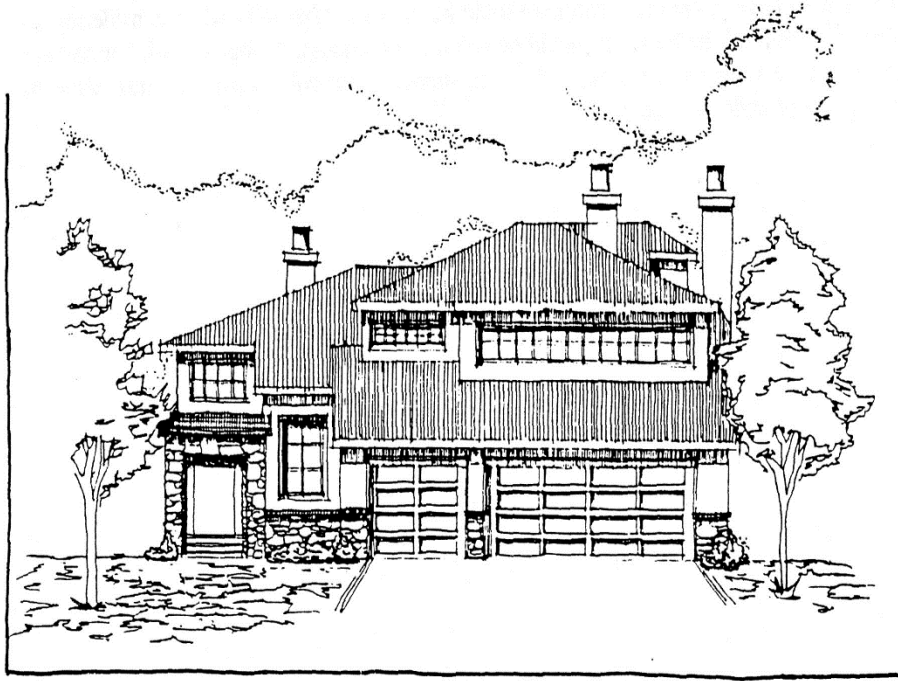
- Exposed mechanical equipment.

It is the intent of these guidelines to establish a consistent architectural expression in the character of *MENIFEE NORTH*. The illustrations shown below are conceptual in nature and are only intended to depict concepts, not actual design. Finalized floor plans and elevations will be determined at a later stage of development.

TYPICAL FRONT ELEVATION



TYPICAL FRONT ELEVATION



1. MATERIALS

The choice of materials is not highly limited at *MENIFEE NORTH*. Their use in appropriate locations and in realistic ways, however, is paramount. The need for all buildings to appear as though they spring from the earth is a demanding task for freshly built, modern structures. Judicious use and mixtures of materials will help the designer succeed in this effort. Because of the rural/desert nature of the project, shiny or flashing materials may be inappropriate. Also, the use of numerous different materials on one structure will tend to clutter the design. It is intended that the buildings at *MENIFEE NORTH* reflect a design idea that uses the materials as a way to express craftsmanship rather than a celebration of the material itself. Simplicity (not to be confused with plainness) will present very well, if the materials are used appropriately and consistently.

2. COLOR PALETTE

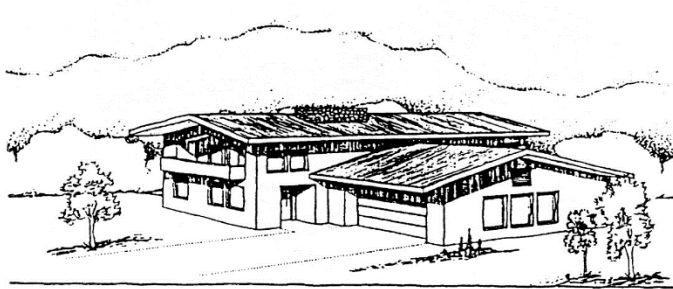
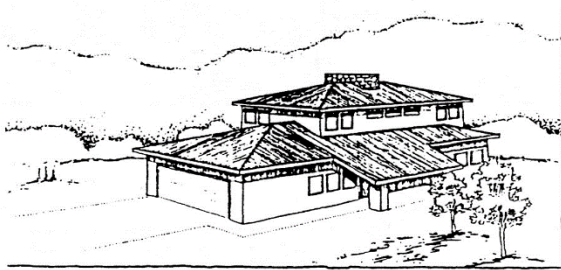
Colors at *MENIFEE NORTH* should be "calm" to reinforce the architectural theme of "oasis". Hot, vibrant colors with large amounts of chroma should be avoided, especially when considering large surfaces. The main body of the building should be colored soft enough to appear cool, but not dark and dreary or muddy. Pastels can be used, if they are subtle. Trim color should contrast with the body color enough to provide an accent.

3. BUILDING MASSING

The emphasis at *MENIFEE NORTH*, in the context of building massing, is on the horizontal line. Building elements which accentuate the horizontal are encouraged, including eave lines, balconies and rails, fenestration patterns, walls and trim lines.

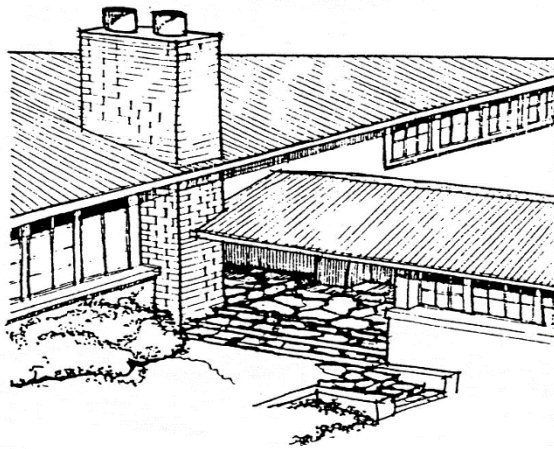
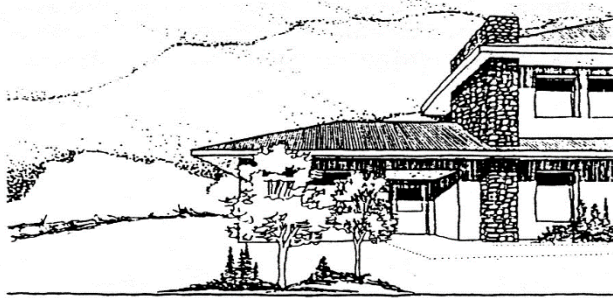
When planning two-story structures, setbacks of enough dimension between one story and that immediately below it are encouraged to disassociate the two vertical planes. Use of roof elements, dressage, and other overhead structures to provide a horizontal break in vertical planes are also encouraged. On small lots, where the narrow frontage may prevent steps on the sides of the units, a front to back break in elevation, as well as varied setbacks along the street, are encouraged.

On commercial projects, loggias, stairways, balconies and walkways are encouraged to provide visual interest as well as to provide pedestrian spaces. Creating elevations that do not vary in plan should be avoided. The transitions from parking areas to the commercial buildings should be logical and shaded if at all possible.



4. ENTRIES

The perception a person receives visually and emotionally from a house or building can set the whole tone for a visit to the space beyond. The creation of a feeling of protection, invitation, and openness all at the same time will allow the appropriate balance to exist between privacy and accessibility. Entries should be protected from the direct impact of the elements but should not be so squeezed or constricted as to feel uninviting or claustrophobic.

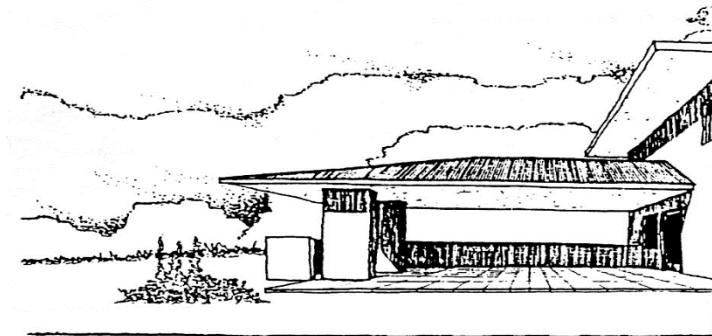
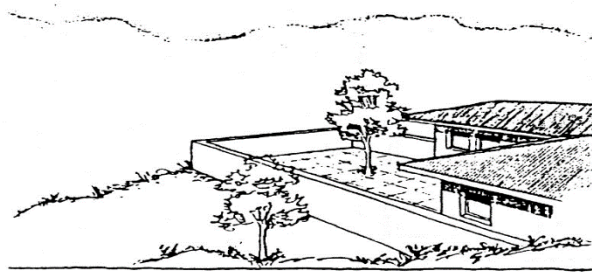


5. SCREENING

The definition between private and public spaces at *MENIFEE NORTH* will occur in a variety of ways. The use of setbacks, garden walls, fences, landscaping and even material changes to define the difference is highly encouraged.

Private courtyards will enhance the feeling of being able to move out of the house or building without moving completely into the public domain. They also can be used to provide a sheltered outdoor space to enjoy some quiet time.

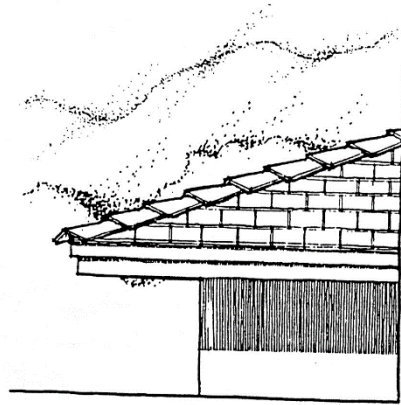
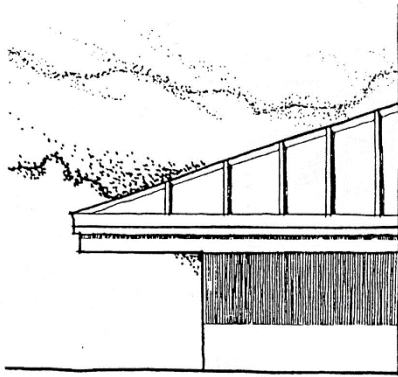
Walls and fences of private yards which are visible to the street or public spaces shall be consistent with the overall theme of the project in which they appear. They also must be architecturally integrated with the community of *MENIFEE NORTH* as a whole. Plastered masonry walls, with or without other elements such as steel tubing, shall align with the design of the project walls discussed in the Landscape Guidelines section of this Specific Plan.



6. ROOFS

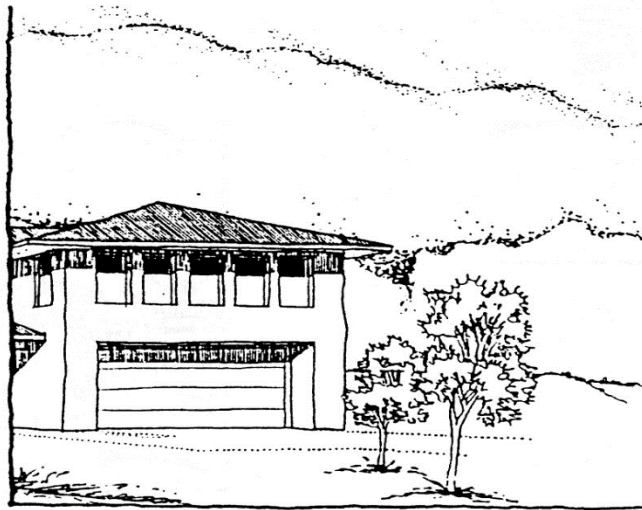
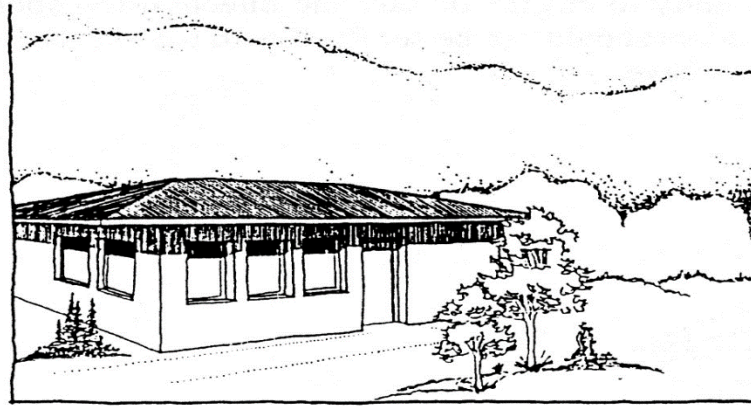
The roof form at *MENIFEE NORTH* is seen as one of the primary form-giving elements. It can be used to help emphasize the horizontal lines of a building, as well as provide shelter from the elements. Because the average annual rainfall is not the major concern of all the elements, the roofs should be low and wide, with slopes predominantly 5:12 or less. Some accent roof areas may be allowed to exceed this slope if they do not represent a large percentage of the total roof area. The overhangs should be a minimum of two feet, so that the shadows they create can be used to shade the walls and windows below them.

The visible roofs of all buildings should appear substantial, as though there is a definite layer between the elements and the protected spaces below. Roofing materials can be either flat clay/concrete tiles or barrel tiles. The tiles will provide a mass which will help slow down heat absorption by the building.



7. WINDOWS

Windows are another basic form-giver to the buildings of *MENIFEE NORTH*. While the primary purpose of windows is to provide light and ventilation, all windows should be protected from the negative impact of the elements, especially the sun. Windows should appear in groups or bands whenever possible, and the detailing of the windows should be consistent around the entire building. Metal windows should have some color to them other than mill finish or natural silver/gray. Views, whether short range, private or public should be exploited.

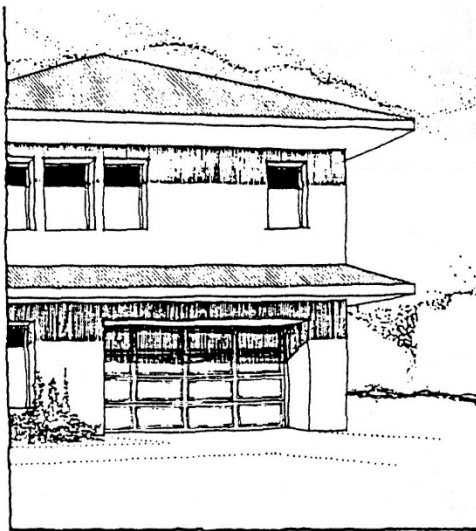


8. DOORS

Doors in the exterior walls of a building at *MENIFEE NORTH* should appear to fit with the style of the other detailing. Regardless of their composition, the coloring and detailing should be consistent. Metal skinned doors should be finished so they do not appear "shiny", unless all doors appear that way. All doors should be considered like windows, and the door's detailing should be reflective of the window detailing. Like windows, doors should be protected from the elements by overhangs, eaves or recesses.

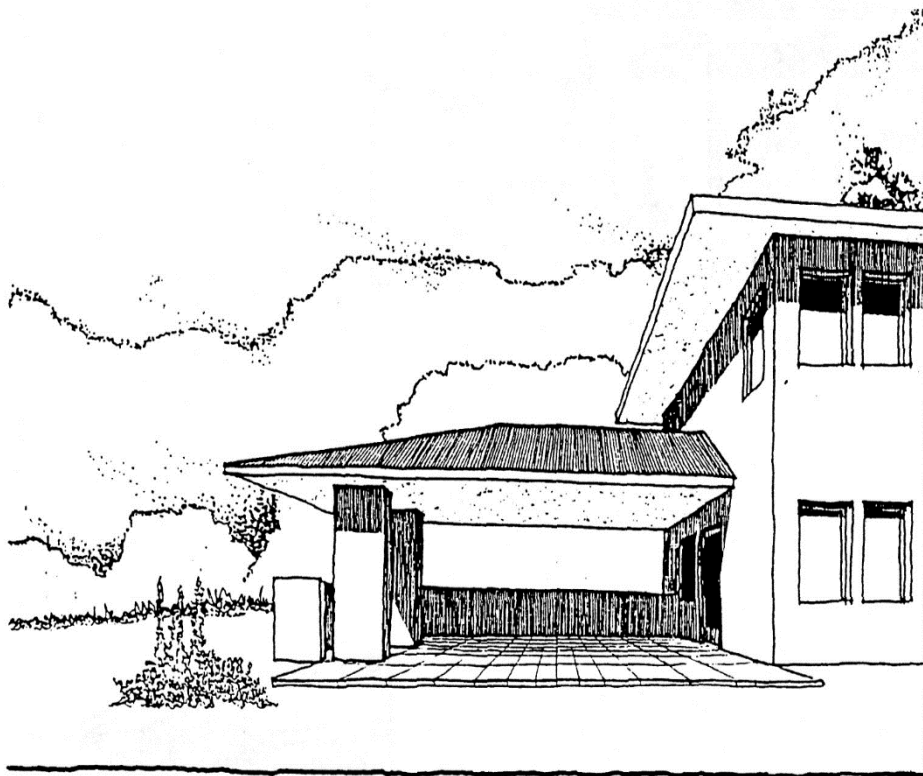
9. GARAGE DOORS

Because the garage on most houses presents a major face to the public, its appearance is very important. The door should be wood, and the detailing should be complimentary to the other doors and detailing. Any opportunity to change or vary the direction the door faces in relation to a street should be exploited. The doors should not be set flush with the wall they appear in, but rather should be recessed or shaded by an eave.



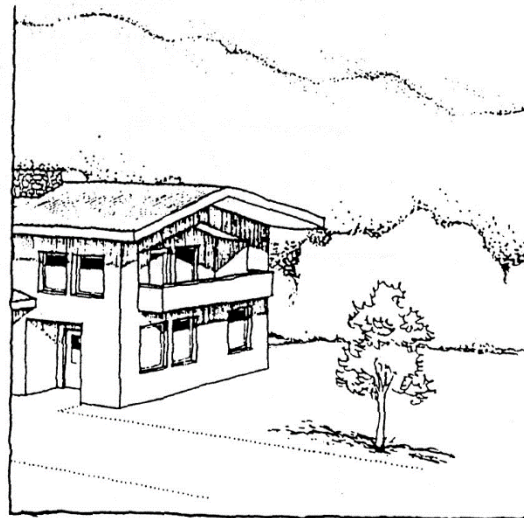
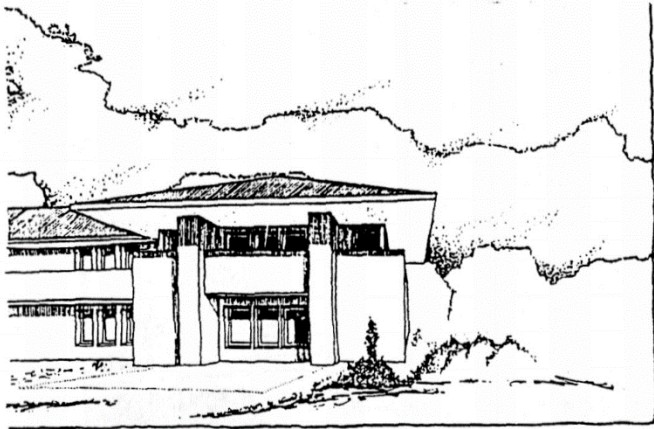
10. PORCHES

A porch, even a small one, can tremendously enhance the feeling of protection from the elements if it is covered or shaded. At *MENIFEE NORTH*, it is desired to have available to all persons some ability to move out of the building but not be out from under a protective covering. Whether it is a place to sit and read, or to set a potted plant, a porch will enhance the feeling of an "oasis".



11. BALCONIES

In second floor conditions, it may be desirable to allow someone to walk outside on a balcony, either to gain a view or to just relax in a secluded area. It is encouraged that the balcony be shaded, protected and in harmony with the entire building design. Details, such as handrails, windows, doors, planters and roofing should enhance and coordinate with the overall design.



12. LOGGIAS

The act of walking between two structures during harsh weather conditions can be enhanced dramatically by the use of a loggia. The covered walkway will reinforce the idea of "shelter" or "oasis", especially if used where a large number of people will congregate. A loggia can be used as a transition between privately owned structures (garage to house for example) or in buildings for public use such as commercial buildings, office buildings and industrial buildings.

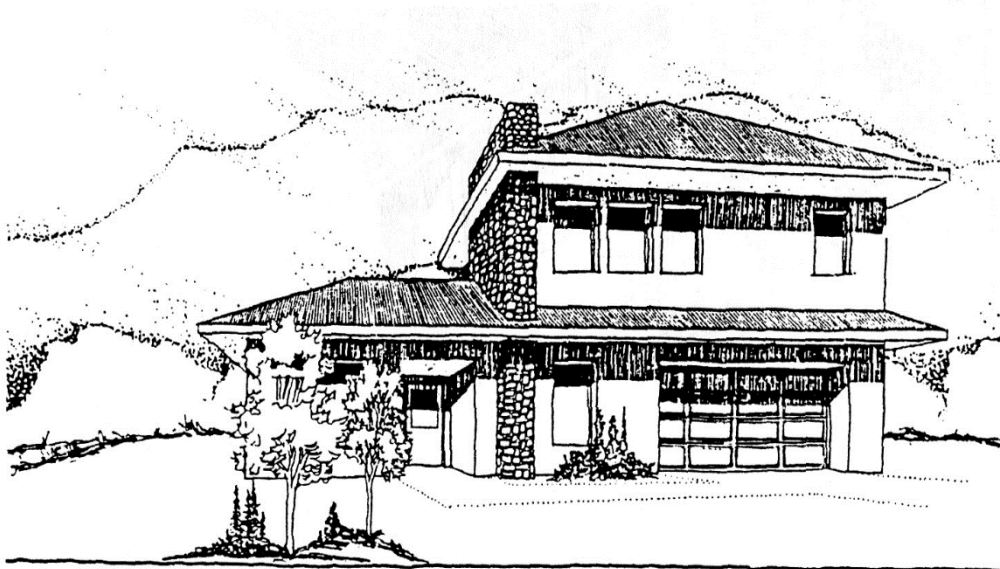


13. TOWERS

MENIFEE NORTH is attempting, even in the higher density parcels, to remain horizontal in overall impression. Towers, vertical billboards, tall signs and/or clock towers are not in keeping with that spirit, except when used in conjunction with landmarks in the community, such as the recreation centers, commercial centers, business parks and industrial parks.

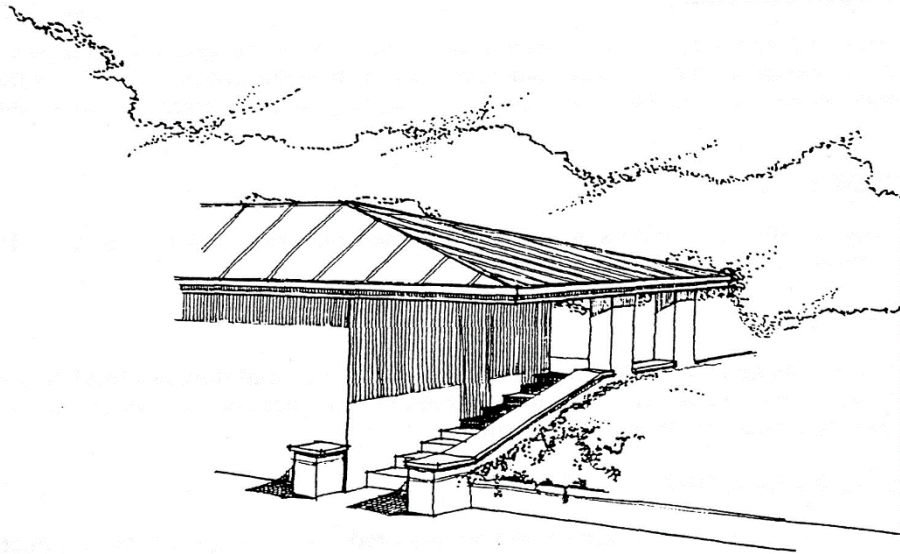
14. CHIMNEYS

The chimney, as the outward symbol of the hearth, should be the anchor for the house visually. To do so, it should appear massive. Indigenous materials are encouraged for use as veneers. Rock, adobe, brick or stone will emphasize the connection to the land, as well as supplying the necessary heft. Plastered boxes containing metal flues are a minimum requirement and they must have detailing applied to enhance them. (While not specifically encouraged, wood clad chimneys may be acceptable if they are coordinated with other architectural elements of the dwelling. They would be reviewed on a case by case basis.) Exposed metal flues or wood trimmed flues are not appropriate.



15. EXTERIOR STAIRS AND STEPS

Changes in elevation at *MENIFEE NORTH* should be handled as subtly as possible. Stairs and ramps should have changes in direction and frequent breaks in rise to soften the impression of overall rise. Steps cast on grade are encouraged to have kneewalls to allow the earth to berm against them. Where appropriate, and if possible, it is recommended that stairs have coverings/roofs to make them appear as part of the building as opposed to an attachment.



16. GUTTERS AND DOWNSPOUTS

It is encouraged that roofs, sloped or "flat", be guttered to collect rainwater at *MENIFEE NORTH*. Where gutters are not used, roof overhangs should be of adequate dimension to minimize dripping of rainwater on the exterior of the dwelling. Rainwater should not fall onto paved areas where people will be walking.

17. SKYLIGHTS AND CLERESTORIES

Skylights and clerestories are encouraged to be used at *MENIFEE NORTH*. They can provide additional light and ventilation for the occupants, as well as add visual interest to the exteriors. Clerestories are preferred over skylights because they can be designed to control heat gain more readily. Skylights, if used, should be as flat as possible in profile, with clear or gray glazing. If they occur on flat roof sections, it is encouraged they be screened from view from public streets.

18. FLASHING AND VENTS

The roof surfaces at *MENIFEE NORTH* are important to the overall design character and should be left as uncluttered as possible. All flashing and vent stacks must be colored to match the surrounding surfaces. Vents should be placed in the roofs where they will provide the least intrusion of the roofscape.

19. MAILBOXES

For single family dwellings, mailboxes will be designed and supplied by the U.S. Postal Service. Further architectural detailing will not be required.

20. UTILITY METERS

At *MENIFEE NORTH*, gas and electric meters should be located so that they are hidden from direct view from the street. They may be placed in "rooms" behind exterior access doors or screening, or they may be hidden by location away from main sight lines.

21. MECHANICAL EQUIPMENT

All mechanical equipment, roof mounted or ground mounted, will be screened by architectural elements as part of the design at *MENIFEE NORTH*. Sound attenuation of mechanical noise also will be necessary, both from neighbors and from within the structure being served by the mechanical equipment. Solar panels of any type must be incorporated into the design of the building on which they are placed. Coloring, materials, and bulk must be coordinated with the overall theme of the *MENIFEE NORTH* community.

22. DESIGN CRITERIA FOR NON-RESIDENTIAL BUILDINGS (COMMERCIAL RETAIL, BUSINESS PARK, LIGHT INDUSTRIAL , AND MIXED USE PLANNING AREAS)

The non-residential buildings of *MENIFEE NORTH* are expected to blend with the overall community in regard to the design guidelines discussed thus far. However, because of their size and varied uses, they should appear distinct from the houses.

When considering massing, the buildings should be stepped, both horizontally and vertically, when possible. Trim lines, detailing and color accents should complement the horizontal nature of the project in general.

The use of loggias, balconies, stairways and other details outlined elsewhere in this document, are encouraged to provide variety in the building massing. Towers or other vertical elements can be used as a focal point.

Mechanical equipment must be screened, and the screening or building parapets used to do this must be an integral design with the main building mass. This screening will be required wherever the mechanical equipment could be seen from adjacent roads, highways, residential areas, and/or adjacent pedestrian areas. Flat roofs are acceptable, but pitched roofs for all or a portion of the buildings are also acceptable.

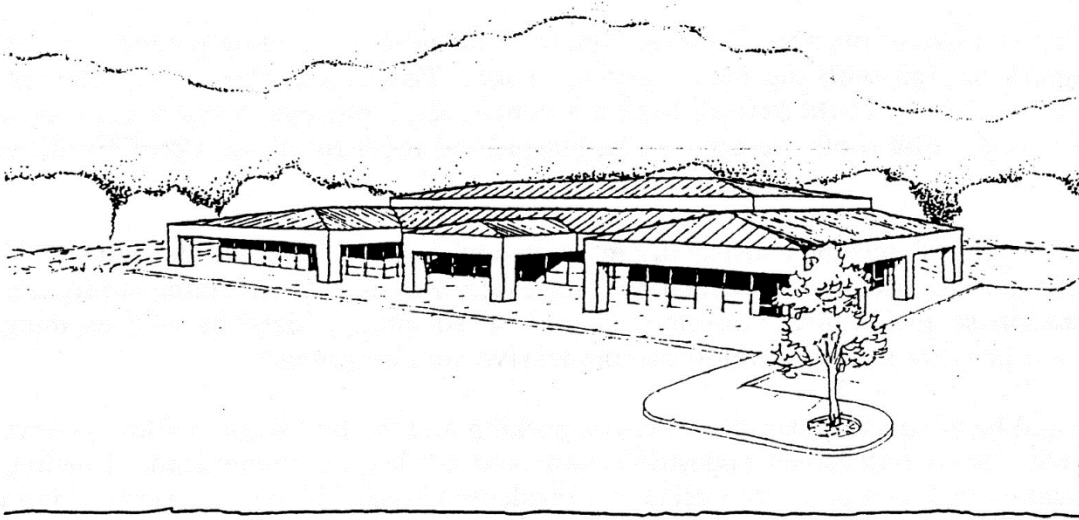
Signage and lighting must be consistently integrated with the building facade, and should be of uniform shape and materials. Retail commercial building exterior design should anticipate signage location, size and coloration so the eventual tenants can provide adequately sized as well as integrated signage. Multi-tenant projects should develop comprehensive sign programs.

There should be adequate separation between parking and the buildings to allow pedestrian flow to be comfortable. Sheltered/shaded pedestrian paths and entries are encouraged. Loading zones should avoid visual exposure to adjacent streets and residential zones. Walls and landscaping can be used to facilitate this screening.

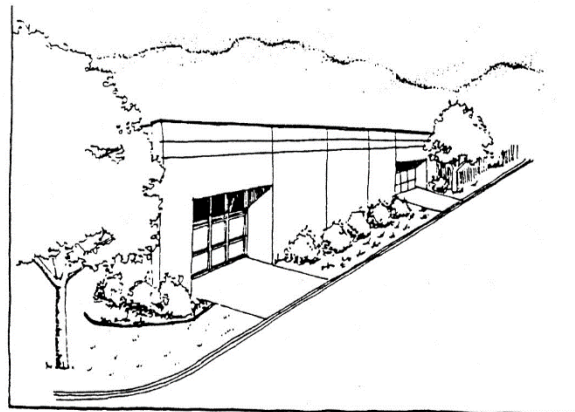
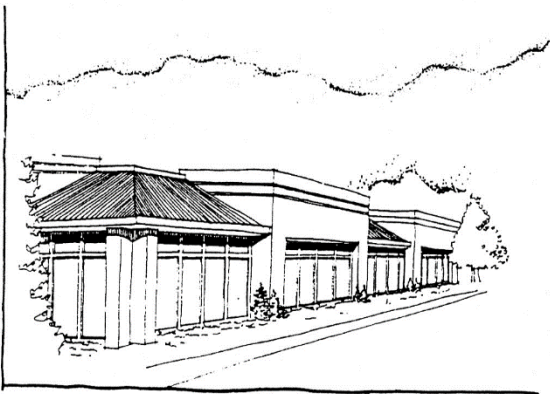
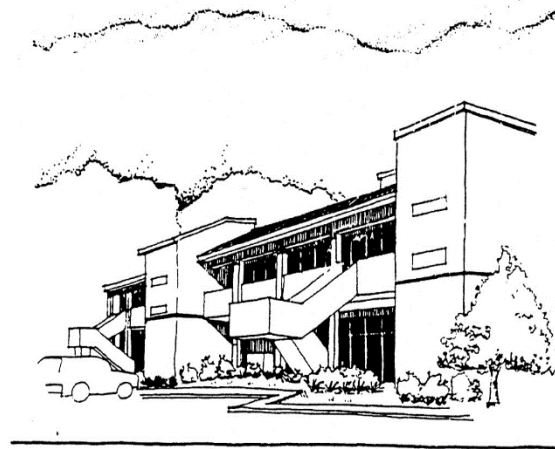
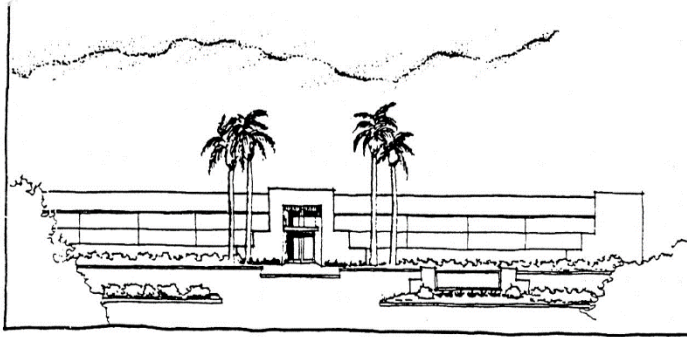
Parking areas should be landscaped, with shading a major consideration. See the landscape guidelines section of this Specific Plan for further requirements. Where it is possible, it is encouraged that service and delivery circulation be separated from the public parking areas.

Reflective glass skins or non-residential buildings should not be used where it will adversely impact the adjacent buildings, especially if the adjacent buildings are residential. In general, large amounts of reflective glass are not in keeping with the overall tone the community is attempting to maintain.

TYPICAL COMMERCIAL RETAIL/BUSINESS PARK ELEVATIONS



TYPICAL COMMERCIAL RETAIL/BUSINESS PARK ELEVATION





AIRPORT LAND USE COMMISSION MEETING
MINUTES
May 11, 2023

DRAFT

5-15-23

COMMISSIONERS PRESENT: John Lyon, Steve Manos, Richard Stewart, Vernon Poole, Michael Lewis (alternate for Steven Stewart), Maartin Rossouw (alternate for Michael Geller)

COMMISSIONERS ABSENT: Russell Betts, Michael Geller, Steven Stewart

2.0 PUBLIC HEARING: CONTINUED ITEMS

None

3.0 PUBLIC HEARING: NEW CASES

3.1 Staff report recommended: ZAP1564MA23 – Stellar Solar Electric (Representative: Frida Mock)
CONSISTENT
– City of Perris Case No. PMT23-00627 (Building Permit). A proposal to construct a 4,910 square foot rooftop solar panel system on an existing 55,650 square foot industrial manufacturing building on 1.94 acres, located easterly of Indian Street, and northerly of Harley Knox Boulevard. (Airport Compatibility Zone C1 of the March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Jackie Vega at (951) 955-0982, or e-mail at javega@rivco.org

Staff recommended at hearing: **CONSISTENT**

ALUC Commission Action: **CONSISTENT (Vote 6-0; Absent: Russell Betts)**

Motion: Richard Stewart
Second: Maartin Rossouw

3.2 Staff report recommended: ZAP1566MA23 – Brew Enterprises II, LLC (Representative: Johnson Aviation Inc.)
CONSISTENT
– City of Perris Case Nos. SPA22-05375 (Specific Plan Amendment), PLN22-00036 (Development Plan Review). A proposal to construct a 58,974 square foot industrial building with mezzanines on 4.01 acres, located southerly of Harley Knox Boulevard, westerly of Perris Boulevard, and easterly of Indian Street. The applicant also proposes to construct a 42,000 square foot solar panel system on the proposed industrial building. The applicant also proposes to amend the Perris Valley Commerce Center Specific Plan Land Use Designation, changing the sites zoning from Commercial (C) to Light Industrial (LI). (Airport Compatibility Zone D of the March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Jackie Vega at (951) 955-0982, or e-mail at javega@rivco.org

Staff recommended at hearing: **CONSISTENT**

ALUC Commission Action: **CONSISTENT (Vote 6-0; Absent: Russell Betts)**

Motion: Michael Lewis
Second: Richard Stewart

VIDEO:

A video recording of the entire proceedings is available on the ALUC website at www.rcaluc.org. If you have any questions please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at basantos@rivco.org

**AIRPORT LAND USE COMMISSION MEETING
MINUTES
May 11, 2023**

4.0 PUBLIC HEARING: MISCELLANEOUS ITEMS

None

5.0 ADMINISTRATIVE ITEMS

5.1 Director's Approvals - Information Only

5.2 Update March Air Reserve Base Compatible Use Study (CUS)

Simon Housman, Project Director for March CUS presented slides reviewing the process, general overview and implementation phase of the March CUS project.

6.0 APPROVAL OF MINUTES

Commissioner Michael Lewis motioned to approve the April 13, 2023 minutes. Seconded by Richard Stewart. (Vote 6-0; Absent Russell Betts)

7.0 ORAL COMMUNICATION ON ANY MATTER NOT ON THE AGENDA

None

8.0 COMMISSIONER'S COMMENTS

None

9.0 ADJOURNMENT

Steve Manos, Chair adjourned the meeting at 10:34 a.m.

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VIDEO:

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