

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM: 3.1

HEARING DATE: October 12, 2023

CASE NUMBER: ZAP1072HR23 – Peak Emerald Acres, LLC (Representative: Sorrento Advisors)

APPROVING JURISDICTION: County of Riverside

JURISDICTION CASE NO: GPA01111 (General Plan Amendment), SP381 (Specific Plan), CZ7774 (Change of Zone), TTM36452 (Tentative Tract Map)

LAND USE PLAN: 2017 Hemet-Ryan Airport Land Use Compatibility Plan

Airport Influence Area: Hemet-Ryan Airport

Land Use Policy: Zones D, E

Noise Levels : Below 55 CNEL contour

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends that the General Plan Amendment, Specific Plan and Change of Zone be found CONSISTENT with the 2017 Hemet-Ryan Airport Land Use Compatibility Plan, and the Tentative Tract Map also be found CONDITIONALLY CONSISTENT, subject to the conditions included herein, and such additional conditions as may be required by the Federal Aviation Administration Obstruction Evaluation Service.

PROJECT DESCRIPTION: A proposal to establish the Emerald Acres Specific Plan allowing for a maximum of 391 residential dwelling units on 335 acres, 4.59 acres of commercial space, 6.6 acres of parkland, and 191.3 acres of conserved open space. The applicant also proposes to amend the General Plan land use designation of the site from Commercial Retail (CR), Low Density Residential (LDR), Medium High Density Residential (MHDR), Rural Mountainous (RM) to the land uses as reflected in the Specific Plan including Medium Density Residential (MDR), High Density Residential (HDR), Commercial Retail (CR), Open Space – Recreation (OS-R), Open Space – Conservation Habitat (OS-CH), Open Space – Conservation (OS-C), and Public Facilities (PF), and change the zoning of the site from Heavy Agriculture 10 Acre Minimum (A-2-10) and Controlled Development Areas (W-2) to Specific Plan and establishing a Specific Plan zoning ordinance that will include the permitted uses and development standards for each Planning Area of the Specific Plan, and establish the Planning Area boundaries of the Specific Plan. The applicant also proposes to divide the 335 acre site into specific lot configurations within each village area to allow for proposed development and resource preservation.

PROJECT LOCATION: The proposed project is located southerly of Florida Avenue, easterly of Calvert Avenue, westerly of California Avenue, and northerly of Stowe Road, approximately 6,900 feet westerly of the easterly terminus of Runway 5-23 at Hemet-Ryan Airport.

BACKGROUND:

Residential Density: The site is located within Compatibility Zones D (20.81 acres) and E (303.40 acres) of the 2017 Hemet-Ryan Airport Influence Area, where Zone D limits residential density to either below 0.4 dwelling units per acre or above 3.0 dwelling units per acre. Zone E does not restrict residential density.

Within the Zone D portion, the project proposes to develop 41 single family residential lots on 10 acres resulting in 4.1 dwelling units per acre, which is consistent with the Zone D residential density criteria of either below 0.4 dwelling units per acre or above 3.0 dwelling units per acre (per Special Policy 2.3).

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zones D or E.

Noise: The Hemet-Ryan Airport Land Use Compatibility Plan depicts the site as being located below the 55 CNEL contour range from aircraft noise. Therefore, no special measures to mitigate aircraft noise are required at this location.

Part 77: The elevation of Hemet-Ryan Airport's Runway 5-23 at its southwesterly terminus is 1,499 feet above mean sea level (AMSL). At a distance of 6,900 feet from the runway to the site, any building with a top point elevation exceeding 1,568 feet AMSL would require notification to the Federal Aviation Administration Obstruction Evaluation Service (FAAOES). The project's site elevation is 1,663 feet AMSL and proposes a maximum building height of 35 feet, resulting in a top point elevation of 1,698 feet AMSL. Therefore, review of the buildings by the FAAOES was required. The applicant has submitted Form 7460-1, and the FAA OES has assigned Aeronautical Study No. 2023-AWP-14223-OE to this project, and is currently in a "work in progress".

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33C). The project is located 6,900 feet from the runway, and therefore would be subject to the above requirement.

The project includes two bioretention basins totaling 451,919 square feet throughout the project site located in Zones D and E, and therefore has the potential to provide food, water, and shelter for hazardous wildlife. Pursuant to the study "Wildlife Hazard Management at Riverside County Airports: Background and Policy", October 2018, by Mead & Hunt, which is the basis of the brochure titled "Airports, Wildlife and Stormwater Management", such basins are permissible in Zones D and E when used in conjunction with appropriate landscaping vegetation so as not to provide food, shelter, nesting, roosting, or water for wildlife. The project has been conditioned to be

consistent with the basin criteria (as well as providing 48-hour draw down of the basin).

Open Area: Pursuant to the Hemet-Ryan Airport Land Use Compatibility Plan, the project site is located within Compatibility Zones D and E. The Compatibility Plan requires projects in Zone D 10 acres or larger to designate 10% of project area as ALUC qualifying open area that could potentially serve as emergency landing areas. Zone E does not require ALUC open area.

Although the portion of the project site is located in Zone D (20.81 acres), open space requirements have already been satisfied under Additional Compatibility Policy Number 2.4.c.

General Plan Amendment/Specific Plan/Change of Zone: The applicant proposes to establish the Emerald Acres Specific Plan allowing for a maximum of 391 residential dwelling units on 335 acres, 4.59 acres of commercial space, 6.6 acres of parkland, and 191.3 acres of conserved open space. The applicant also proposes to amend the General Plan land use designation of the site from Commercial Retail (CR), Low Density Residential (LDR), Medium High Density Residential (MHDR), Rural Mountainous (RM) to the land uses as reflected in the Specific Plan including Medium Density Residential (MDR), High Density Residential (HDR), Commercial Retail (CR), Open Space – Recreation (OS-R), Open Space – Conservation Habitat (OS-CH), Open Space – Conservation (OS-C), and Public Facilities (PF), and change the zoning of the site from Heavy Agriculture 10 Acre Minimum (A-2-10) and Controlled Development Areas (W-2) to Specific Plan and establishing a Specific Plan zoning ordinance that will include the permitted uses and development standards for each Planning Area of the Specific Plan, and establish the Planning Area boundaries of the Specific Plan. The proposed land use designations located in the portions of the project that is in Zone E is consistent as Zone E does not restrict residential density or non-residential intensity.

A portion of the project proposes Medium Density Residential, 2 to 5 dwelling units per acre, located in Zone D, where density is restricted to either below 0.4 dwelling units per acre or above 3.0 dwelling units per acre. There is the potential for a project to propose a density range between 2.0 to 2.9 dwelling units per acre which would be consistent with the Medium Density Residential density range, but inconsistent with Zone D density criteria. Therefore, language has been included in the Specific Plan land use table footnote that specifically requires all projects must be consistent with the underlying airport land use compatibility criteria. As such, the proposed amendments would be consistent with the Compatibility Plan as long as the underlying development's is consistent with the compatibility criteria.

CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.

- (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, outdoor production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Highly noise-sensitive nonresidential uses.
 - (f) Any use which results in a hazard to flight, including physical (e.g. tall objects), visual, and electronic forms of interference with the safety of aircraft operations.
3. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property and be recorded as a deed notice.
4. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the stormwater basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

**THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS**

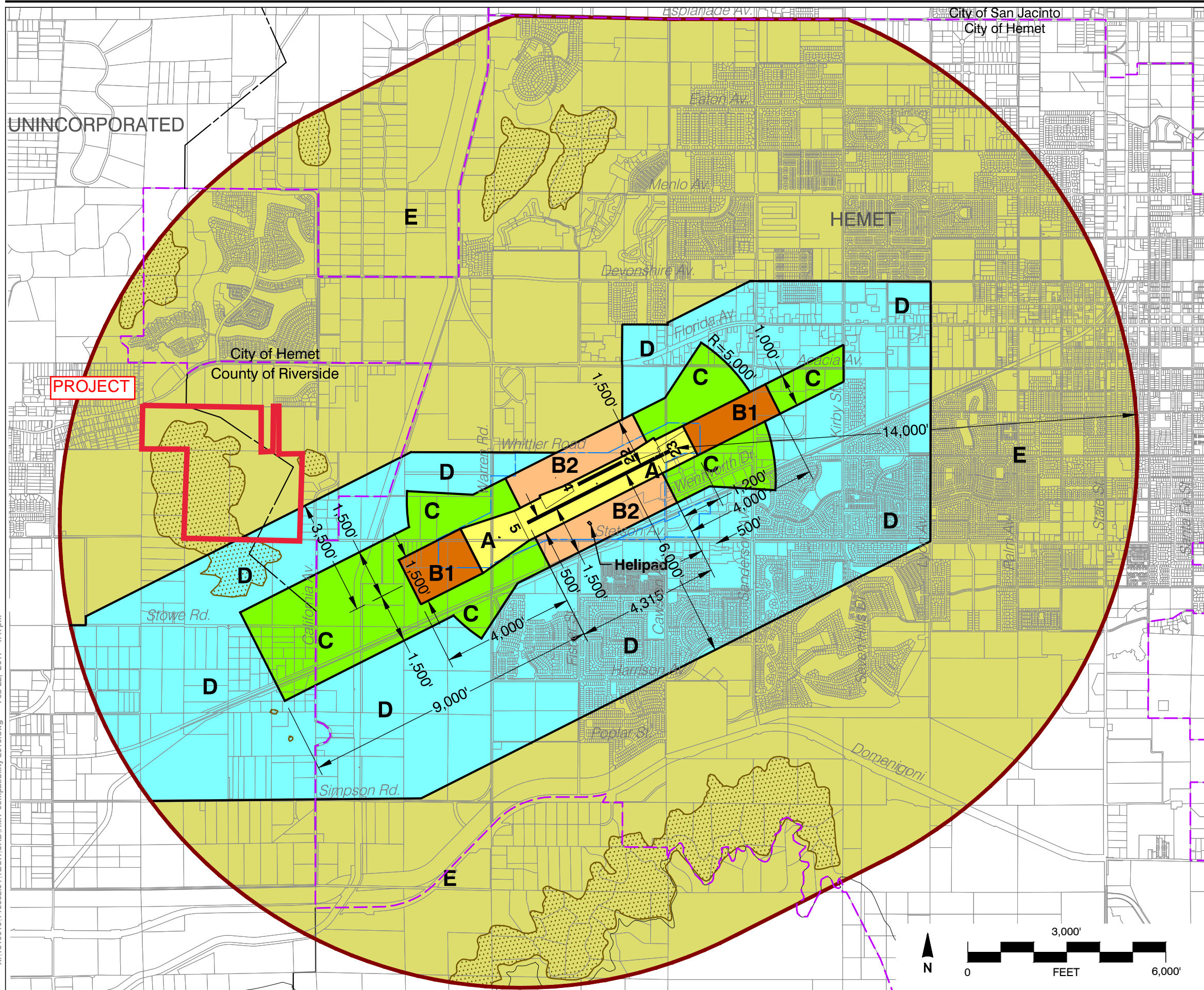
**PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES**



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



Legend

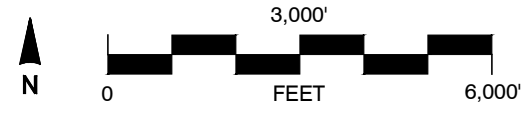
- Compatibility Zones**
- Airport Influence Area Boundary
 - Zone A
 - Zone B1
 - Zone B2
 - Zone C
 - Zone D
 - Zone E
 - Height Review Overlay Zone
- Boundary Lines**
- Airport Property Line
 - City Limits
 - City Sphere of Influence

Note
 Airport Influence Area boundary measured from a point 200 feet beyond ends of proposed future 4,815 foot runway in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from ends and centerlines of existing 4,315 foot runway.

Riverside County
Airport Land Use Commission
Hemet-Ryan Airport
Land Use Compatibility Plan
 (Adopted February 9, 2017)

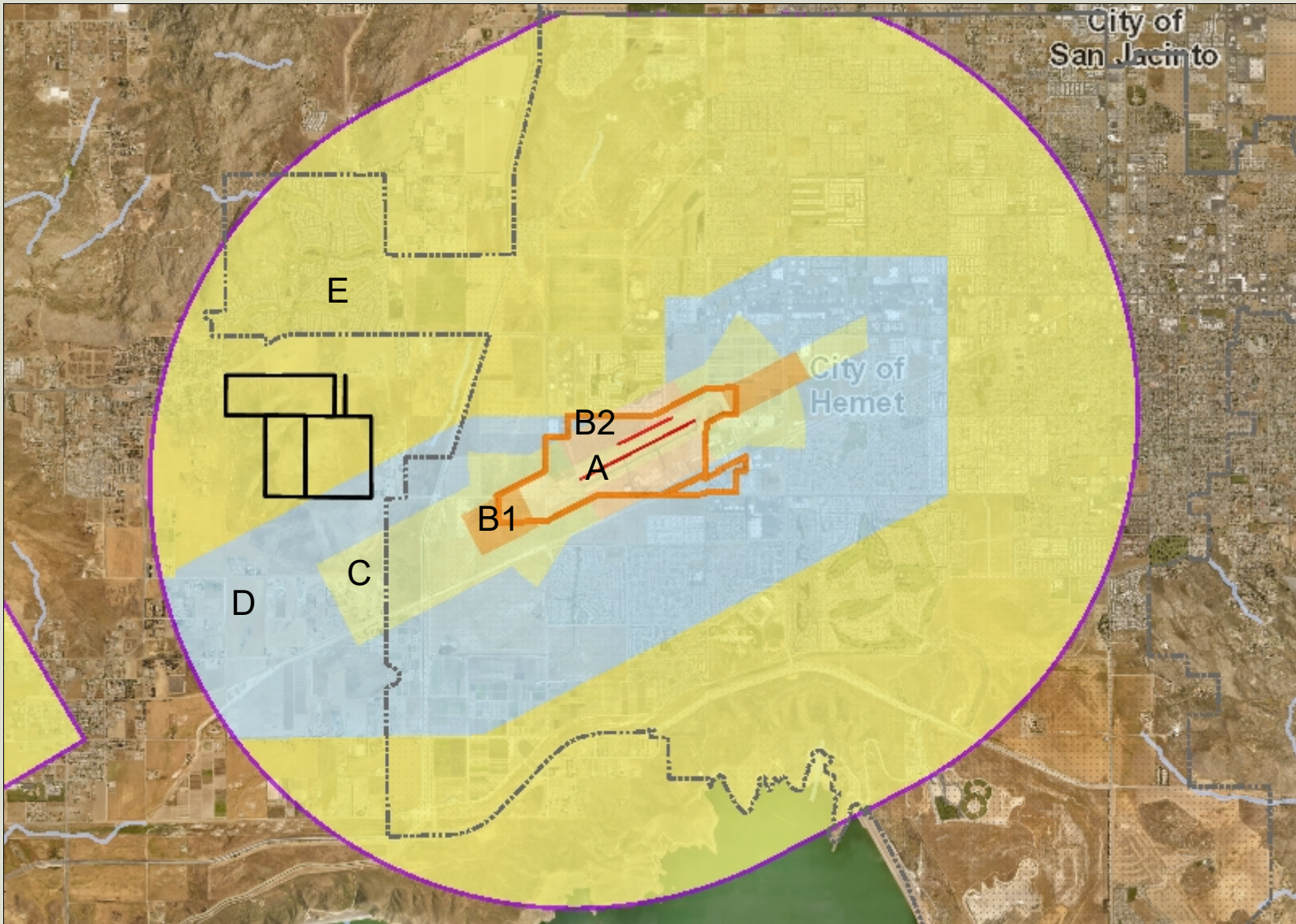
Map HR-1

Compatibility Map
Hemet-Ryan Airport



X:\1819010\113532.01\TECH\CAD\HMT-compatibility 2016.dwg Feb 22, 2017 - 4:47pm

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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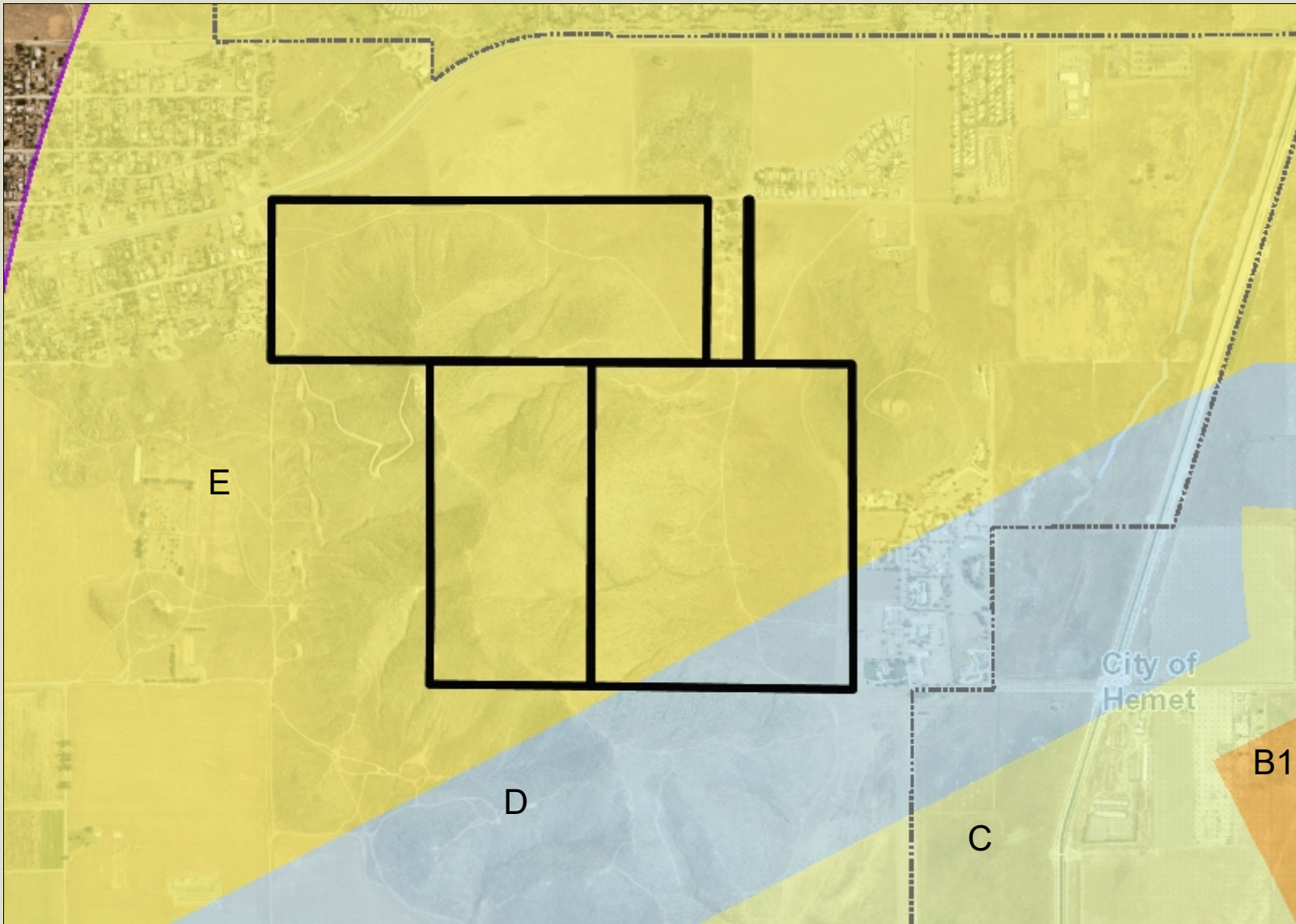


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Notes

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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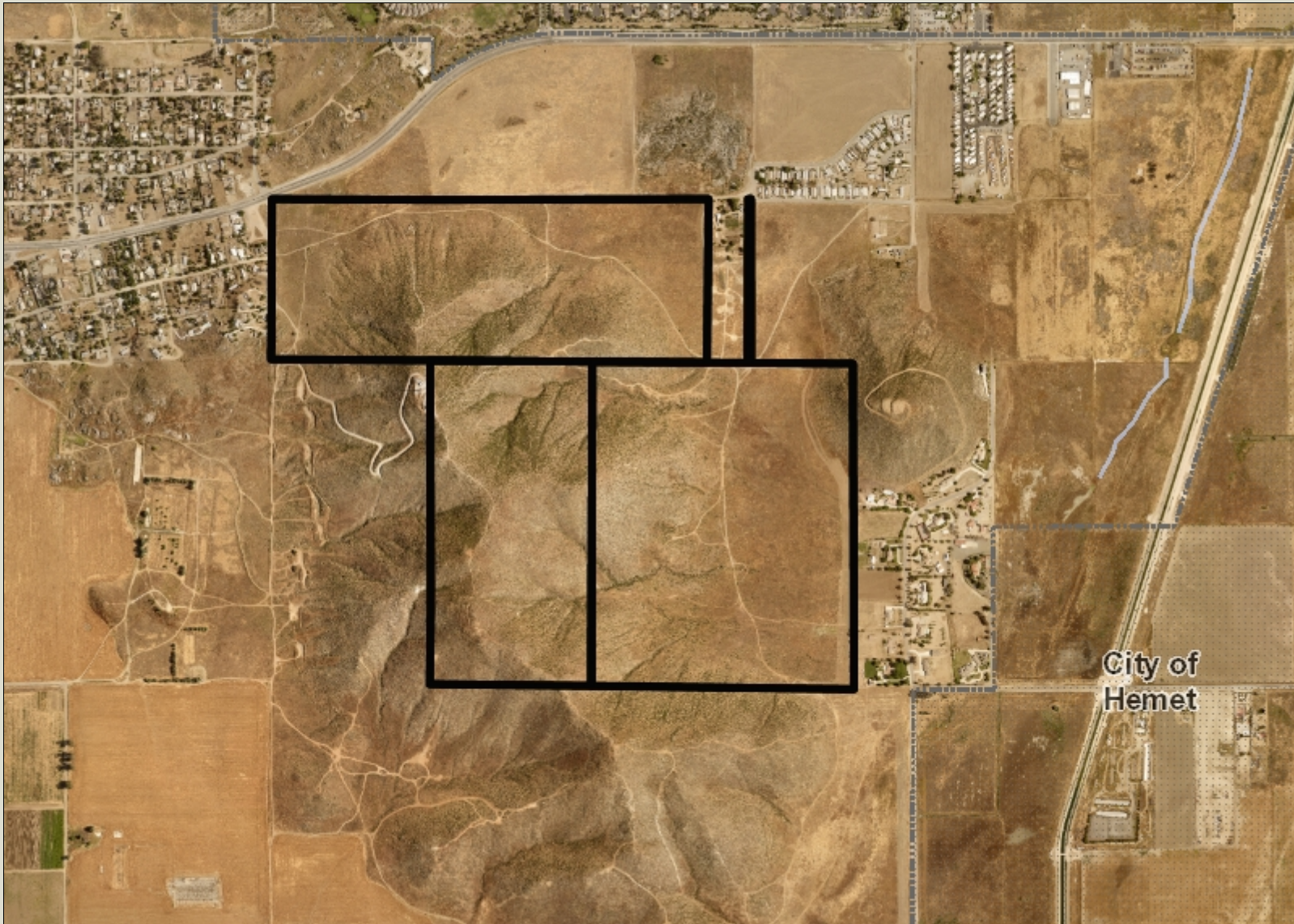


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Notes

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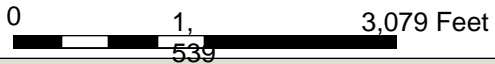


Legend

-  Blueline Streams
-  City Areas
-  World Street Map



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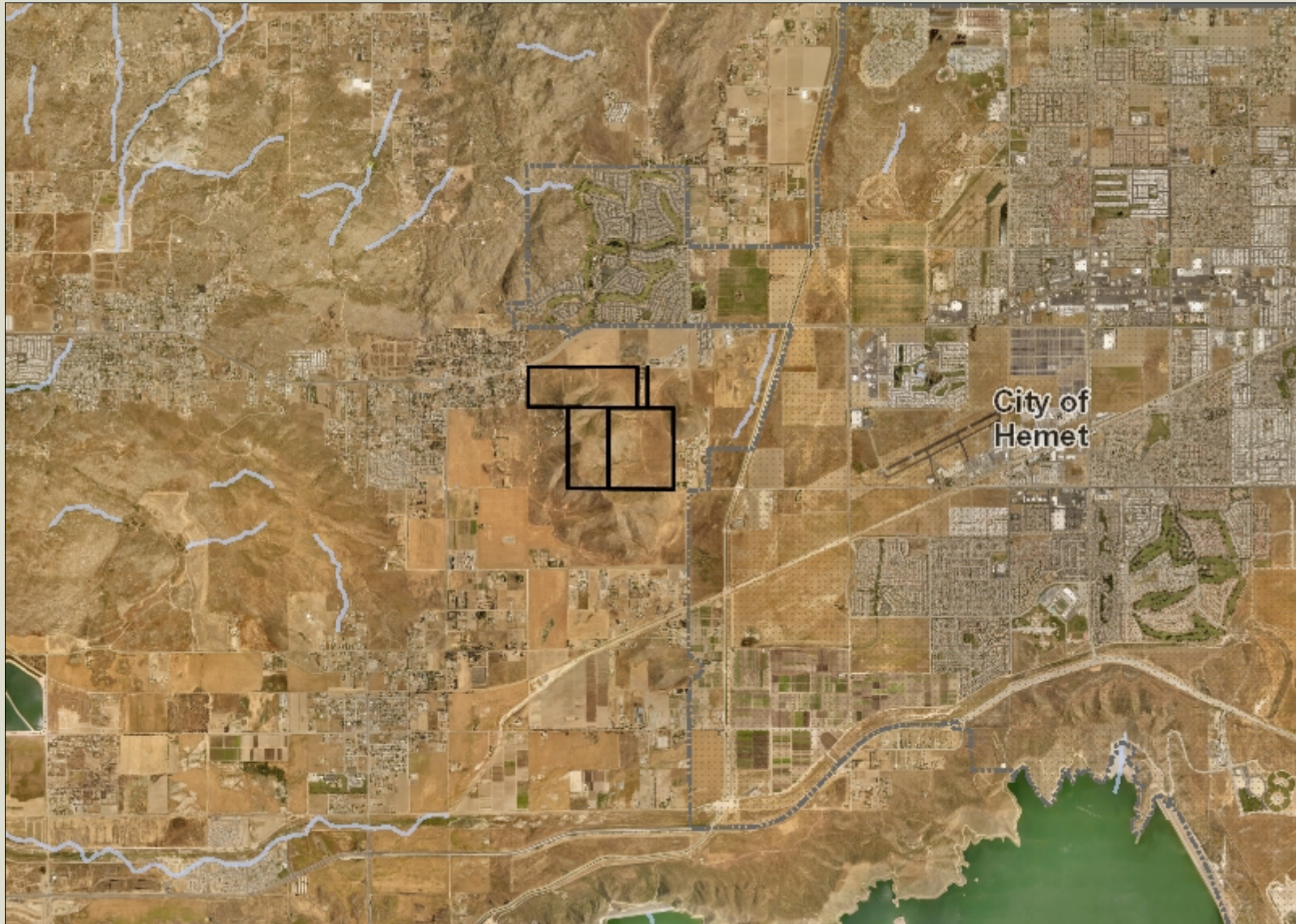


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Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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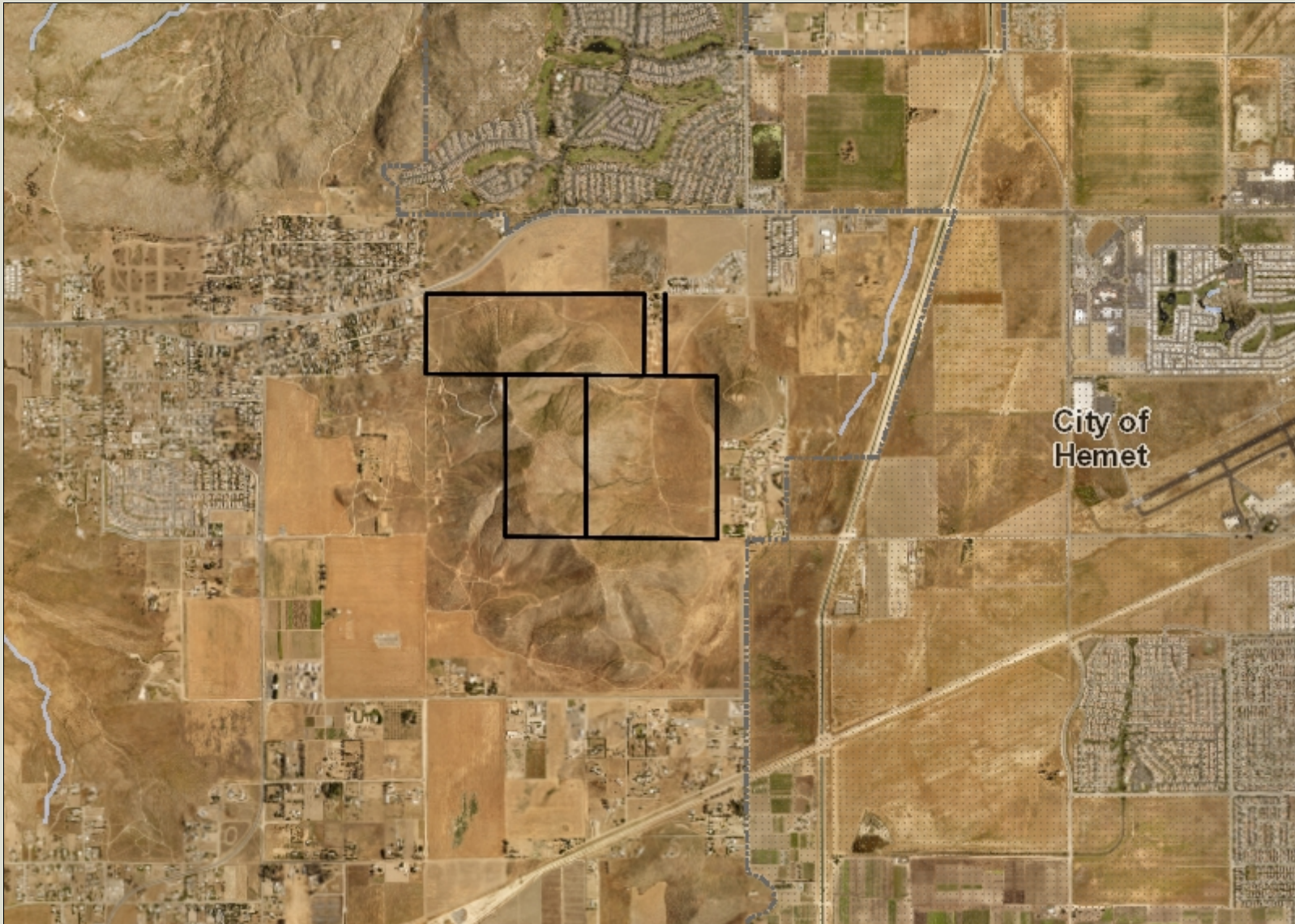
Notes




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-  Blueline Streams
-  City Areas
-  World Street Map

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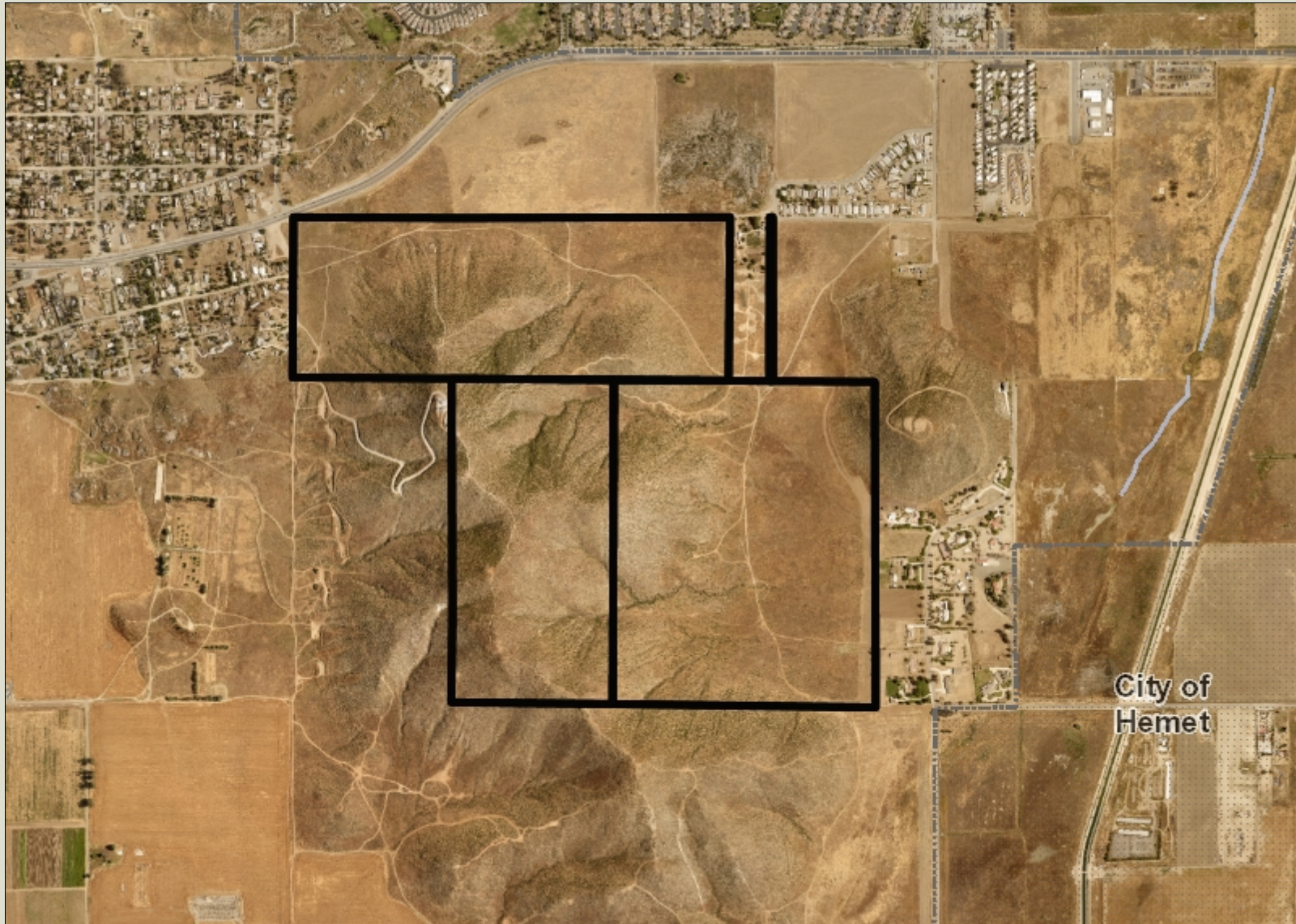
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Notes

Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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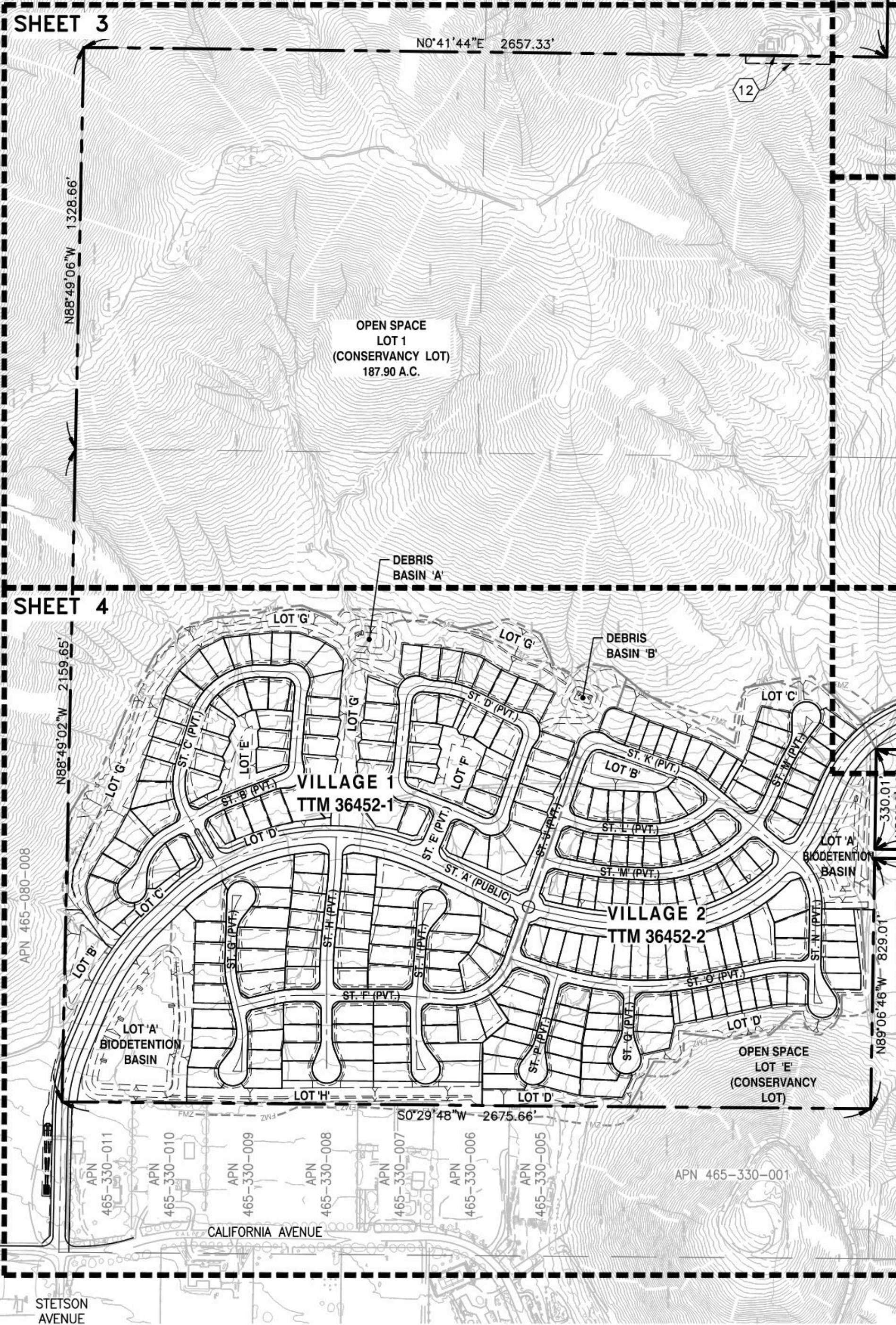
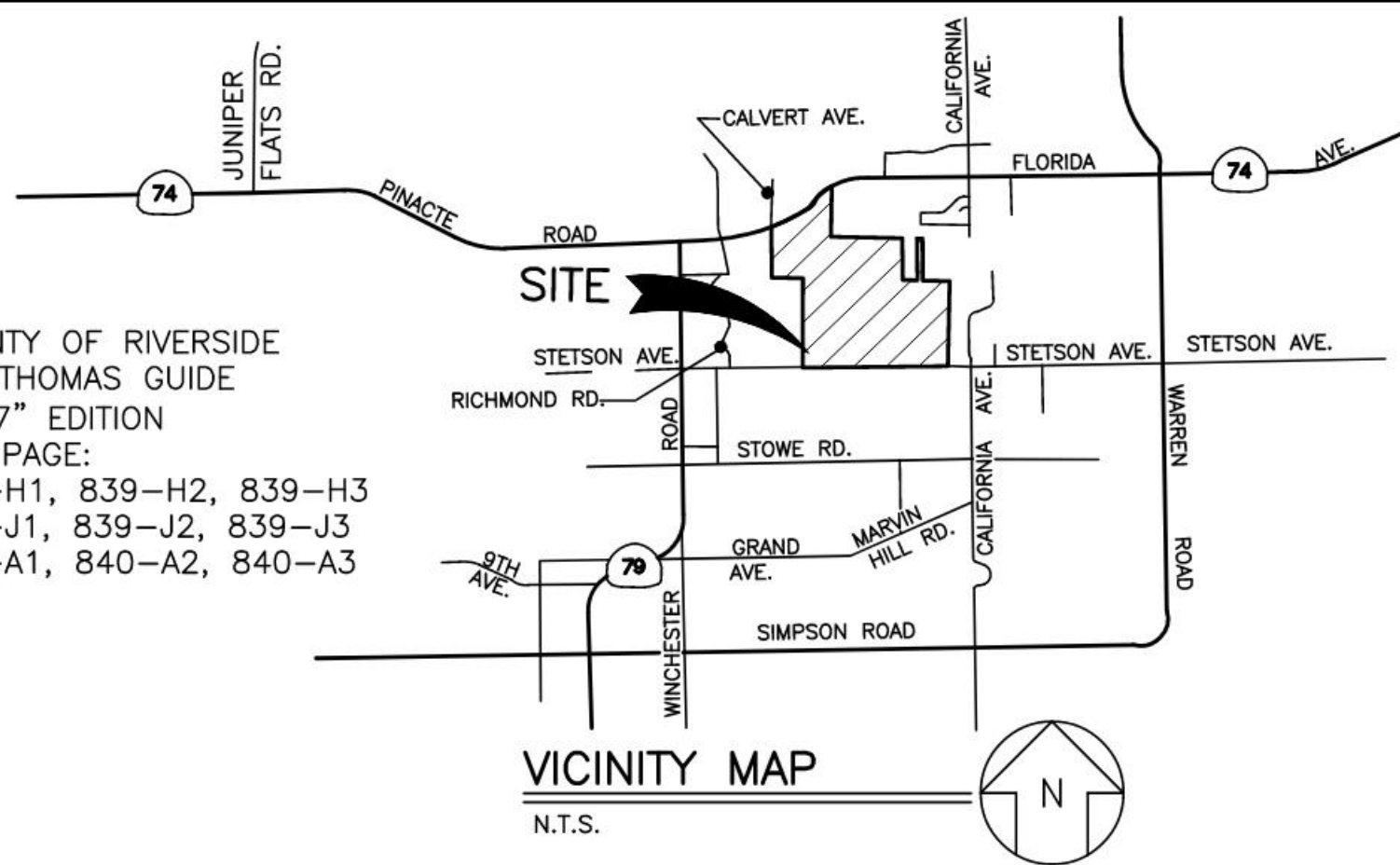


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Notes

COUNTY OF RIVERSIDE
THE THOMAS GUIDE
"2007" EDITION
MAP PAGE:
839-H1, 839-H2, 839-H3
839-J1, 839-J2, 839-J3
840-A1, 840-A2, 840-A3



LEGAL DESCRIPTION

THE SOUTH HALF OF THE NORTHWEST QUARTER AND THE EAST HALF OF THE SOUTHWEST QUARTER, TOGETHER WITH A PORTION OF THE NORTHWEST QUARTER OF THE NORTHWEST QUARTER AND A PORTION OF THE SOUTHWEST QUARTER OF THE NORTHEAST QUARTER AND A PORTION OF THE SOUTHEAST QUARTER, ALL OF SECTION 14, TOWNSHIP 5 SOUTH, RANGE 2 WEST, SAN BERNARDINO BASE AND MERIDIAN, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, ACCORDING TO OFFICIAL PLAT THEREOF, FILED IN THE DISTRICT LAND OFFICE DECEMBER 12, 1869.

BENCHMARK

THE BASIS OF ELEVATIONS FOR THIS SURVEY IS THE NGS CONTROL STATION 463+41 RESET (FD 2" BOSTAMPED "463+31 RESET"), AS SHOWN NGS DATA SHEET.
ELEV = 1502.37' (NAVD88)

LEGEND

- PROJECT BOUNDARY
- VILLAGE BOUNDARY
- PROPOSED STREET CENTERLINE
- 100' FUEL MODE ZONE
- PROPOSED SLOPE
- PROPOSED SEWER
- PROPOSED WATER
- PROPOSED RECYCLED WATER
- PROPOSED STORM DRAIN
- PROPOSED STORM DRAIN STRUCTURES
- PROPOSED PAD ELEVATION
- CONCRETE BROW DITCH
- PROP. CONTOUR
- DAYLIGHT
- PROP. RET. WALL

LAND USE TABLES

VILLAGE 1: 6,000 S.F. SINGLE-FAMILY LOTS

TYPE	LOT COUNT	LOT #	ACREAGE	OWNED BY	MAINTAINED* BY
RESIDENTIAL LOT	143	1-143	26.76	HOMEOWNER	HOMEOWNER
OPEN SPACE LOT	1	A	3.71	H.O.A.	COUNTY-BASIN LMAD-SLOPES
	3	C,D,H	2.02	H.O.A.	H.O.A.
	2	E,F	1.67	H.O.A.	VALLEYWIDE DISTRICT
PRIVATE STREETS	---	---	17.74	H.O.A.	H.O.A.
	---	---	3.93	COUNTY	COUNTY
PUBLIC STREET (STREET A)	---	---	3.93	COUNTY	COUNTY
WATER PUMP AND RECYCLED WATER BOOSTER STATION	1	B	0.83	E.M.W.D.	E.M.W.D.
TOTAL	151		65.52		

VILLAGE 2: 5,000 S.F. SINGLE-FAMILY LOTS

TYPE	LOT COUNT	LOT #	ACREAGE	OWNED BY	MAINTAINED* BY
RESIDENTIAL LOT	105	1-105	16.31	HOMEOWNER	HOMEOWNER
OPEN SPACE LOT	1	A	1.73	H.O.A.	COUNTY-BASIN LMAD-SLOPES
	1	B	0.66	H.O.A.	VALLEYWIDE DISTRICT
	2	C,D	3.36	H.O.A.	H.O.A.
	1	E	4.52	CONSERVANCY	CONSERVANCY
PRIVATE STREETS	---	---	5.19	H.O.A.	H.O.A.
PUBLIC STREET (STREET A)	---	---	2.82	COUNTY	COUNTY
TOTAL	110		34.59		

VILLAGE 3: 5,000 S.F. SINGLE-FAMILY LOTS

TYPE	LOT COUNT	LOT #	ACREAGE	OWNED BY	MAINTAINED* BY
RESIDENTIAL LOT	19	1-19	2.76	HOMEOWNER	HOMEOWNER
OPEN SPACE LOT	1	A	6.63	H.O.A.	VALLEYWIDE DISTRICT
	1	B	2.84	H.O.A.	COUNTY-BASIN LMAD-SLOPES
	2	C,D	2.25	H.O.A.	H.O.A-SLOPES
	2	E,F	0.68	H.O.A.	H.O.A-SLOPES
	---	---	0.89	H.O.A.	H.O.A.
PRIVATE STREETS	---	---	0.89	H.O.A.	H.O.A.
PUBLIC STREET (STREET A)	---	---	5.12	COUNTY	COUNTY
TOTAL	25		21.17		

VILLAGE 4: SINGLE-FAMILY AND COMMERCIAL LOTS

TYPE	LOT COUNT	LOT #	ACREAGE	OWNED BY	MAINTAINED* BY
RESIDENTIAL LOT	123	1-123	6.53	HOMEOWNER	HOMEOWNER
COMMERCIAL LOT	2	B & C	4.59	OWNER	OWNER
OPEN SPACE LOT	1	A	2.30	H.O.A.	COUNTY BASIN
	1	D	0.90	H.O.A.	VALLEYWIDE DISTRICT
	1	E	0.65	H.O.A.	H.O.A-SLOPES
	5	F,G,H,I,J	6.22	H.O.A.	H.O.A-SLOPES
	---	---	6.07	H.O.A.	H.O.A.
PRIVATE STREETS	---	---	6.07	H.O.A.	H.O.A.
PUBLIC STREETS (CALVERT AVE. & STREET A)	---	---	5.30	COUNTY	COUNTY
TOTAL	133		32.56		

LOT 1:

TYPE	LOT COUNT	LOT #	ACREAGE	OWNED BY	MAINTAINED* BY
OPEN SPACE LOT	1	1	187.90	CONSERVANCY	CONSERVANCY

GRAND TOTAL	LOT COUNT
RESIDENTIAL (SINGLE-FAMILY)	391
COMMERCIAL	2
OPEN SPACE LOT	26
OPEN SPACE LOT (LOT 1 & LOT 'E' IN VIL. 2 (CONSERVANCY))	2
WATER PUMP AND RECYCLED WATER BOOSTER STATION	1
TOTAL	422

ENGINEER:

SAN DIEGUITO ENGINEERING
1911 PALOMAR OAKS WAY, SUITE 200
CARLSBAD, CA 92008
(PHONE) 858-345-1238
(FAX) 858-345-1151
(ATTN.) JOSHUA D. REEVES, P.E.

OWNER/APPLICANT:

PEAK EMERALD ACRES, LLC
9595 WILSHIRE BOULEVARD, STE. 710
BEVERLY HILLS, CA 90212
(PHONE) 310-273-4187
(ATTN.) GREG SCOTT, MANAGING MEMBER

SOILS ENGINEER:

GEOCON
41571 CORNING PL., SUITE 101
MURRIETA, CA 92562
(PHONE) 951-304-2300
(FAX) 951-304-2392
(ATTN.) BOB RUSSELL

ASSESSORS PARCEL NUMBER

465-040-005, 006, 014 & 016

ACREAGE

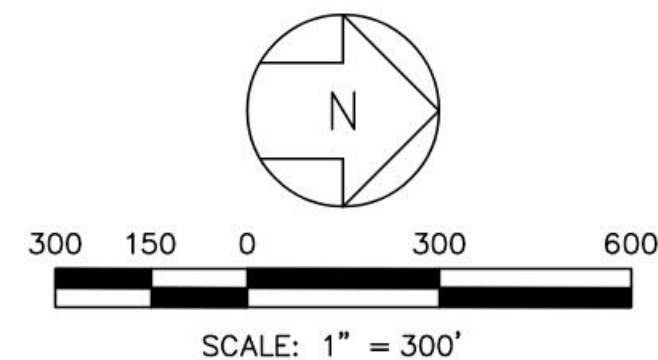
NET 154.19 AC. GROSS 332.56 AC.
NOTE: NET ACREAGES EXCLUDES CONSERVANCY LOT 1 AND VILLAGE 2 LOT 'E'

GENERAL NOTE

1. **THIS PROJECT:**
 - A) CONTAINS THE ENTIRE CONTIGUOUS OWNERSHIP OF THE LAND DIVIDER;
 - B) IS NOT LOCATED WITHIN A SPECIFIC PLAN; HOWEVER A SPECIFIC PLAN FOR THE SUBJECT PROPERTY IS BEING CONCURRENTLY PROPOSED WITH THIS TENTATIVE MAP TRACT.
 - C) PORTIONS OF THE SUBJECT PROPERTY ARE LOCATED WITHIN CSAs 80 (HOMELAND) AND 146 (LAKEVIEW/ROMOLAND/HOMELAND).
 - D) PORTIONS OF THE PROJECT SITE ARE SUBJECT TO MODERATE LIQUEFACTION RISKS AND SUSCEPTIBLE TO SUBSIDENCE.
 - E) IS NOT SUBJECT TO INUNDATION, OVERFLOW OR FLOOD HAZARDS;
 - F) HAS NO KNOWN WELLS ON THE SITE OR WITHIN 200 FEET.
 - G) VILLAGE 4 IS 123 RESIDENTIAL LOTS AND TWO COMMERCIAL LOT.
 - H) TOTAL SINGLE-FAMILY RESIDENTIAL LOTS IN VILLAGES 1-4 IS 391 DWELLING LOTS. TTM 36452-1 (144 DWELLING LOTS) TTM 36452-2 (105 DWELLING LOTS) TTM 36452-3 (19 DWELLING LOTS) TTM 36452-4 (123 DWELLING LOTS) TTM 36452-4, COMMERCIAL LOT 1 IS: 2.45 ACRES & LOT 2 IS: 2.14 ACRES
 - I) ESTIMATED EARTHWORK QUANTITIES: 534,170 C.Y. CUT, 652,500 C.Y. FILL; IMPORT 118,330 C.Y.
2. **LAND USE:**
 - EXISTING: VACANT
 - PROPOSED: SINGLE FAMILY RESIDENTIAL AND COMMERCIAL
 - ADJACENT PROPERTY: NORTH: VACANT EAST: VACANT/SINGLE FAMILY RESIDENTIAL SOUTH: VACANT WEST: VACANT/SINGLE FAMILY RESIDENTIAL
3. **ZONING:**
 - EXISTING: W-2 AND A-2-10
 - PROPOSED: SP
 - ADJACENT PROPERTY: NORTH: W-2 AND R-T-20000 WEST: R-R AND A-2-10
4. **GENERAL PLAN**
 - EXISTING: CR, RM, LDR, AND MHR
 - PROPOSED: MDR, HDR, OS-C, OS-R, AND PF
5. **SOURCE OF TOPOGRAPHY**
COUNTY OF RIVERSIDE GIS TOPO
6. **SCHEDULE OF IMPROVEMENTS**
SCHEDULE "A"
7. **UTILITIES:**
 - ALL UTILITIES TO BE UNDERGROUND
 - CABLE TV: TIME WARNER 951-766-4270
 - ELECTRIC: SOUTHERN CALIFORNIA EDISON 951-928-8251
 - GAS: SOUTHERN CALIFORNIA GAS 951-928-2808
 - SEWER: EASTERN MUNICIPAL WATER DISTRICT 951-928-3777
 - WATER: EASTERN MUNICIPAL WATER DISTRICT 951-928-3777
 - TELEPHONE: VERIZON OF CALIFORNIA 951-929-9491
8. **SCHOOL DISTRICT:**
HEMET UNIFIED SCHOOL DISTRICT
9. **FLOOD ZONE:**
FLOOD ZONE C
10. **EASEMENTS**
 - THE FOLLOWING EASEMENTS ARE LISTED PER TITLE REPORT:
 - AN EASEMENT FOR PUBLIC UTILITIES IN FAVOR OF CALIFORNIA ELECTRIC COMPANY PER DOCUMENT RECORDED NOVEMBER 6, 1959 AS INST. NO. 95212, OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA. (APPROXIMATE LOCATION OF CENTERLINE PLOTTED-ILLEGIBLE DOCUMENT)
 - 20' WIDE EASEMENT FOR PUBLIC UTILITIES IN FAVOR OF EASTERN MUNICIPAL WATER DISTRICT PER DEED RECORDED NOVEMBER 21, 1989 AS INST. NO. 408486, OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA. (PLOTTED)
 - 26' WIDE PRIVATE EASEMENT FOR ACCESS ROAD BY PRESCRIPTION IN FAVOR OF LYLE AND GERTRUDE PLATT PER DOCUMENT RECORDED JULY 8, 1990 AS INST. NO. 249231, OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA. (PLOTTED)
 - 20' WIDE EASEMENT FOR PUBLIC UTILITIES IN FAVOR OF EASTERN MUNICIPAL WATER DISTRICT PER DEED RECORDED MAY 2, 1995 AS INST. NO. 137711, OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA. (PLOTTED)

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SHEET 2	STREET SECTIONS AND DETAILS
SHEET 3-6	GRADING AND UTILITIES
SHEET 7-9	STREET 'A' ALIGNMENT AND GRADE
SHEET 10	CALVERT AVENUE ALIGNMENT AND GRADE

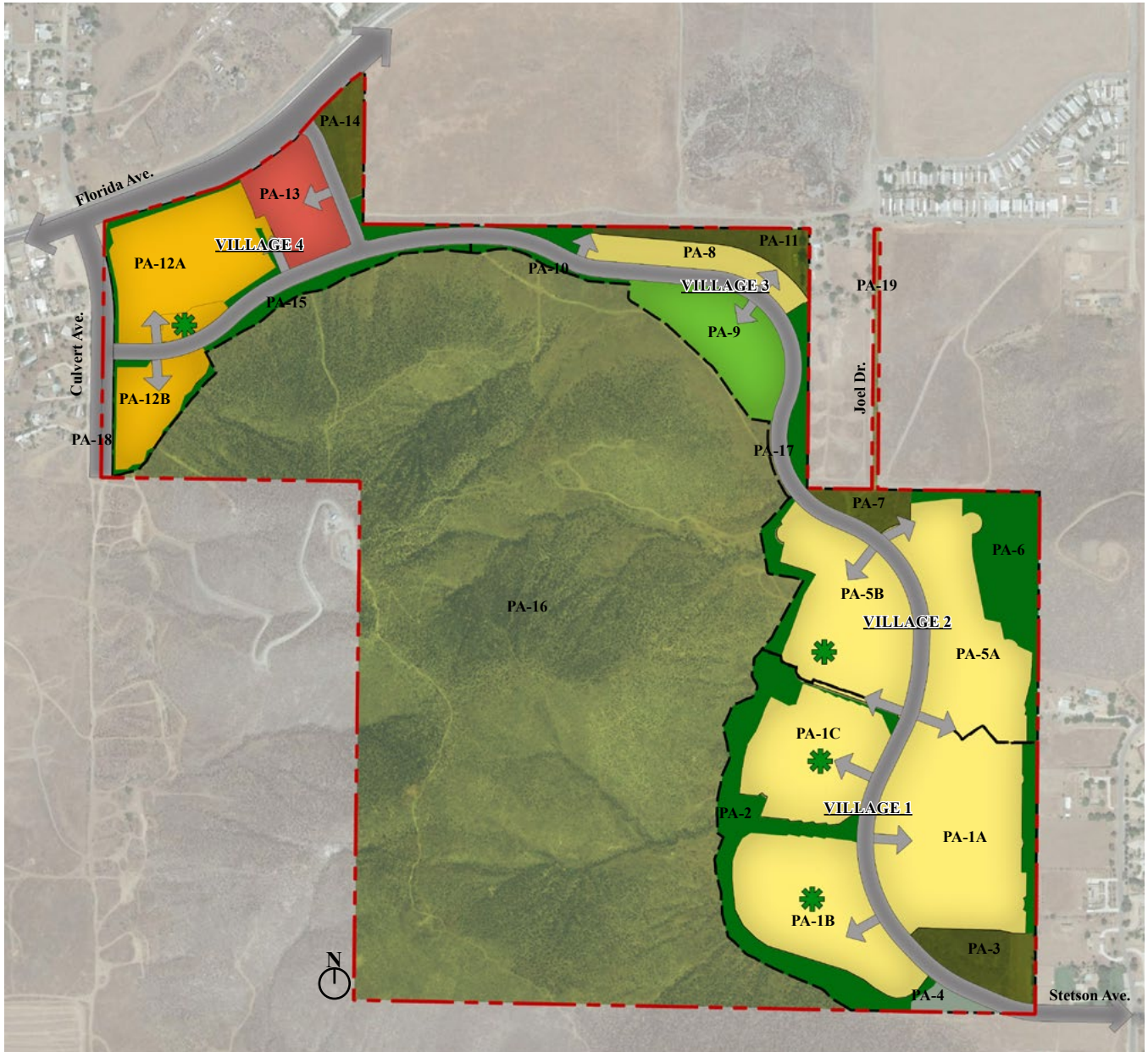


REVISIONS	DRAWN BY:	CHECKED BY:

APPROVED BY: _____
REGISTERED CIVIL ENGINEER NO. _____
DATE: _____
APPROVED BY: _____
DATE: _____
COUNTY OF RIVERSIDE

SCALE: AS NOTED
DATE: NOV. 2022
BENCHMARK AT THE NORTHWEST CORNER OF THE INTERSECTION OF THE SAN DIEGO CANAL AND STETSON AVE. AT THE INLET SIDE OF THE STETSON AVE SIPHON, 30 FT WEST OF THE CENTERLINE SAN DIEGO CANAL. DATE: 4-18-2022. ELEV: 1502.37' (NAVD88)

IN THE COUNTY OF RIVERSIDE
EMERALD ACRES
TENTATIVE TRACT NO. 36452
DWG. NO. 1
OF 10 SHEETS
FILE NO. _____
W.O. _____ F.B. _____



LEGEND

- Medium Density Residential (6,000 sf Min. Lots)
- Medium Density Residential (5,000 sf Min. Lots)
- High Density Residential (Attached/Detached Motorcourt)
- Commercial Retail

- Open Space-Recreation (Neighborhood Park)
- Open Space-Conservation (Slopes)
- Open Space-Conservation (WQMP Basin)
- Open Space-Conservation Habitat (Natural Open Space)

- Public Facilities
- Backbone Road
- Village Recreation Area
- Project Boundary

EXHIBIT 2-1. LAND USE PLAN

TABLE 2-1. DETAILED LAND USE SUMMARY

Planning Area	Village	Land Use	Acres	Density Range	Target Density	Target Units
1A, 1B, 1C	Village 1	Medium Density Residential 6,000 s.f. Min. Lots	35.8	2-5 du/ac ¹	4.0 du/ac	144
2		Open Space - Conservation Slopes	10.6			
3		Open Space - Conservation WQMP Basin	3.7			
4		Public Facilities	0.8			
5A, 5B	Village 2	Medium Density Residential 5,000 s.f. Min. Lots	22.5	2-5 du/ac	4.7 du/ac	105
6		Open Space - Conservation Slopes	7.8			
7		Open Space - Conservation WQMP Basin	1.7			
8	Village 3	Medium Density Residential 5,000 s.f. Min. Lots	3.7	2-5 du/ac	5.0 du/ac	19
9		Open Space - Recreation Neighborhood Park	6.6			
10		Open Space - Conservation Slopes	2.8			
11		Open Space - Conservation WQMP Basin	2.8			
12A, 12B	Village 4	High Density Residential Attached or Detached Motorcourt	13.5	8-14 du/ac	9.1 du/ac	123
13		Commercial Retail	5.0			
14		Open Space - Conservation WQMP Basin	2.8			
15		Open Space - Conservation Slopes	6.8			
16		Open Space - Conservation Habitat Natural Open Space	191.3			
17		Backbone Roads	15.4			
18		Existing Road Easement	1.3			
19		Joel Drive Extension	0.9			
TOTAL		PROJECT TOTAL	335.5	-	1.2 du/ac	391

EMERALD ACRES

SPECIFIC PLAN NO.381

DRAFT

SEPTEMBER 08, 2023
RIVERSIDE COUNTY, CALIFORNIA



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EMERALD ACRES

SPECIFIC PLAN NO. 381



Prepared for

COUNTY OF RIVERSIDE
4080 LEMON STREET, 12TH FLOOR
RIVERSIDE, CA 92501
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In Consultation with

SORRENTO ADVISORS

Project Design Consultants

SAN DIEGUITO ENGINEERING, INC.

C2 COLLABORATIVE

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SECTION 1: INTRODUCTION

1.1. PROJECT SUMMARY

The Emerald Acres Specific Plan is a 335.5-acre master planned community located just west of the City of Hemet, along and highly visible from the Florida Avenue (State Highway 74) corridor in the Harvest Valley/Winchester area of unincorporated Riverside County, California. The Emerald Acres community provides residents the opportunity to live in a picturesque setting, which accommodates and incorporates the surrounding natural open space with striking views, within a high-quality community, with innovative housing types, a commercial component, and recreational amenities.

With a total of 391 homes within four distinct Villages, Emerald Acres assist in meeting the region's increasing need for attainable housing and is designed to appeal to a variety of residents, including young professionals, members of the local labor force, and first-time home buyers. As described in Table 1-1, General Land Use Summary, single-family residential uses within Villages 1, 2, and 3 include lot sizes of 6,000 and 5,000 square feet. Village 4 provides for the development of 123 attached and detached motorcourts residences. Planning Area 13 in Village 4 is designated for commercial uses, which will allow for retail commercial uses which serve both the residents of the Emerald Acres community and travelers on Florida Avenue/California Highway 74.

The community's recreational amenities further establish Emerald Acres as a desirable master-planned community in western Riverside County because they are designed to ensure that a wide array of recreational opportunities, as well as natural open space, is available to all residential neighborhoods within the community. The hierarchy of recreational opportunities includes a public 6.6-acre Neighborhood Park, Village Recreation Areas, Multi-Use Trail, and natural open space. The 6.6-acre Neighborhood Park, which serves as the community focal point, may provide a basketball court, baseball field, soccer field, picnic grove, tot lot, restroom facilities, and on-site parking for the residents of Emerald Acres and the surrounding communities. The Village Recreational Areas provide open-space, recreational venues of a more passive and intimate nature, within individual neighborhoods.

Residents also enjoy the recreational, decomposed granite Multi-Use Trail, which provides community access from the southeastern entry of the community and traverses the entirety of Emerald Acres within the Street A right-of-way. Access points to the trail network are located within each Village, which encourages community-wide pedestrian walkability and bicycle mobility.

To preserve the rugged and scenic hillsides, 191.3 acres of natural open space is provided within the Emerald Acres community that will remain undisturbed as conservation habitat. These undisturbed areas create a striking visual backdrop and lend a distinct character to the Emerald Acres community.

The overall architectural theme of Emerald Acres is drawn from the historic architectural styles used throughout southern California, including California Bungalow, Spanish, Craftsman, and Monterey styles. These architectural styles complement the natural open space and echo the surrounding natural scenery. Architectural styles within the community require high-quality construction and are suitable for the wide variety of residential home types. Homes within the community feature design components which complement the environmental characteristics of the area, such as balconies, covered porches, and courtyards.

A cohesive landscape concept enhances the Emerald Acres architectural theme and creates a sense of community identity for residents and visitors. All private residences, public areas, streetscapes, and community landscaping will include automatic, irrigation systems to reduce the amount of water used for irrigation.

TABLE 1-1. GENERAL LAND USE SUMMARY

Land Use ⁽²⁾	Acres	Density Range ⁽²⁾	Target Density	Target Units
Residential				
Medium Density Residential 6,000 s.f. Min. Lots	35.8	2-5 du/ac ⁽¹⁾⁽²⁾	4.0 du/ac	144
Medium Density Residential 5,000 s.f. Min. Lots	25.9	2-5 du/ac	4.8 du/ac	124
High Density Residential	13.5	8-14 du/ac	9.1 du/ac	123
RESIDENTIAL SUBTOTAL	75.2	-	5.2 du/ac	391
Non-residential				
Commercial Retail	5.0			
Open Space-Recreation Neighborhood Park	6.6			
Open Space- Conservation Habitat Natural Open Space	191.3			
Open Space-Conservation Slopes/Fire Fuel Mods/Basins	39.0			
Public Facility	0.8			
Project Circulation	15.4			
Existing Road Easement	1.3			
Joel Drive Extension	0.9			
NON-RESIDENTIAL SUBTOTAL	260.3			
PROJECT TOTAL	335.5	-	1.2 du/ac	391

¹ du/ac = dwelling units per acre

² Densities, intensities, and prohibited uses shall be consistent with criteria of the Hemet-Ryan Airport Land Use Compatibility Plan, including applicable Countywide criteria as may exist at the time of project review. If the project is located in the Hemet-Ryan Airport Compatibility zones, please see ALUCP and County of Riverside Planning staff for the most up-to-date regulations.

The Vehicular Circulation Plan for Emerald Acres is designed to provide safe and convenient vehicular access throughout the community. Primary access to the community is obtained from Florida Avenue, a Modified Expressway (184’ right-of-way), via Street E (74’ right-of-way) and from Calvert Avenue, a Modified Secondary Highway (112’ right-of-way), via Street A (74’ right-of-way). South of the boundary between Village 2 and Village 3, Street A transitions to a public 85-foot Modified Collector road that includes an additional 5’ of sidewalk and 6’ of parkway, further providing primary access to Emerald Acres via Stetson Avenue. An off-site future road which connects Street A north to Florida Avenue is not a part of the Specific Plan and may be constructed by others; however, the Emerald Acres Specific Plan accommodates the future road.

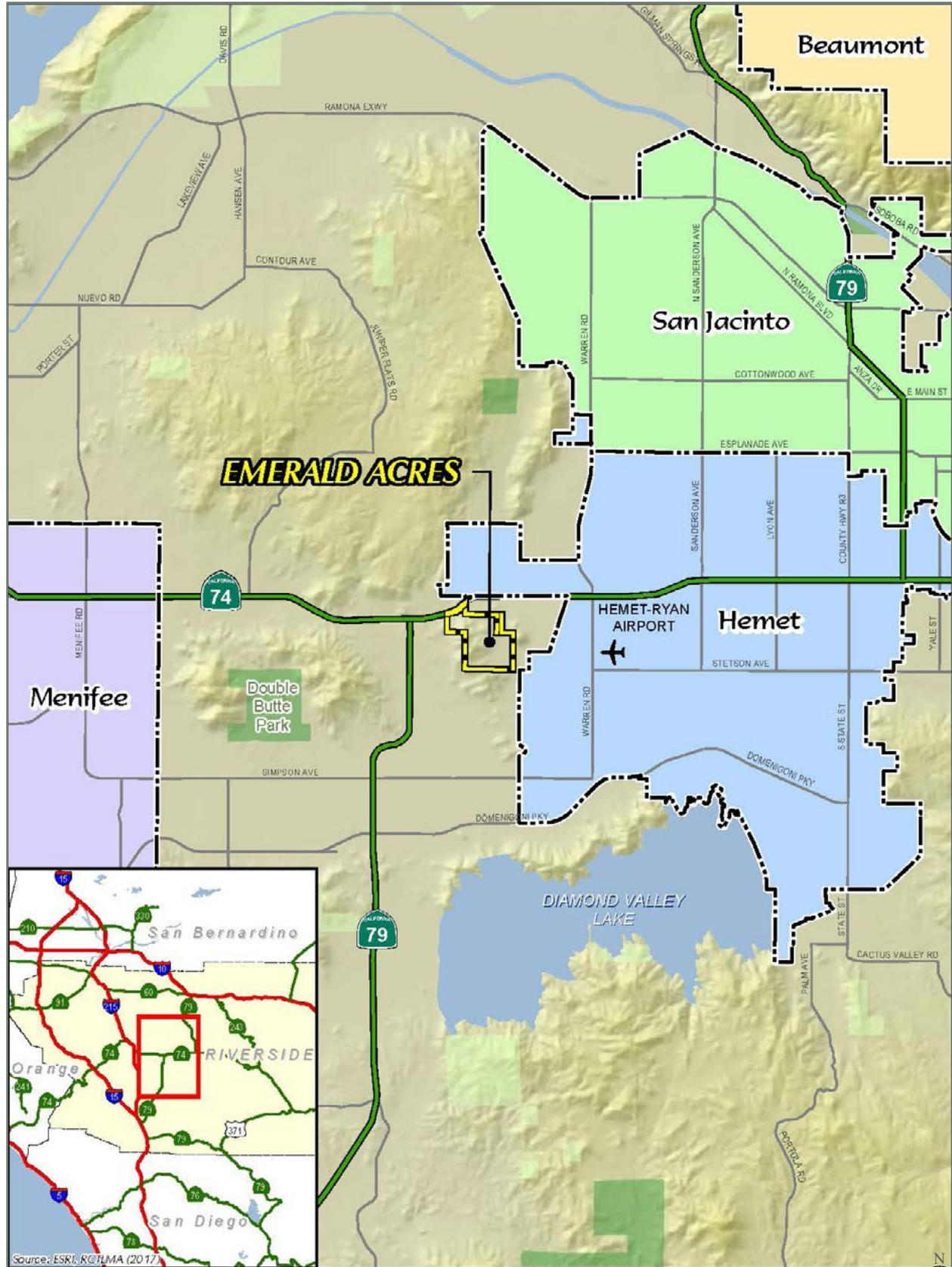


EXHIBIT 1-1. REGIONAL MAP

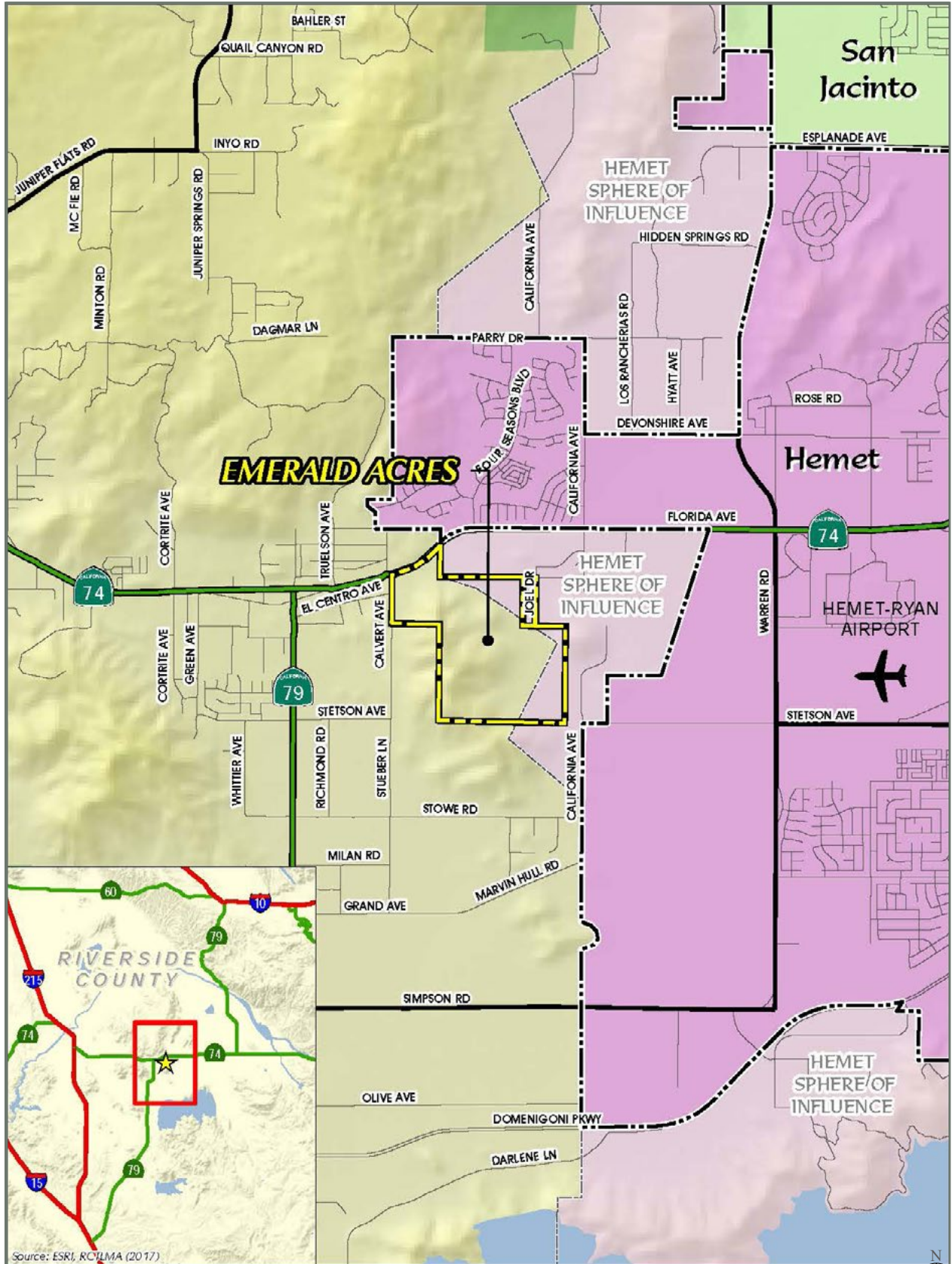


EXHIBIT 1-2. VICINITY MAP



EXHIBIT 1-3. AERIAL PHOTOGRAPH



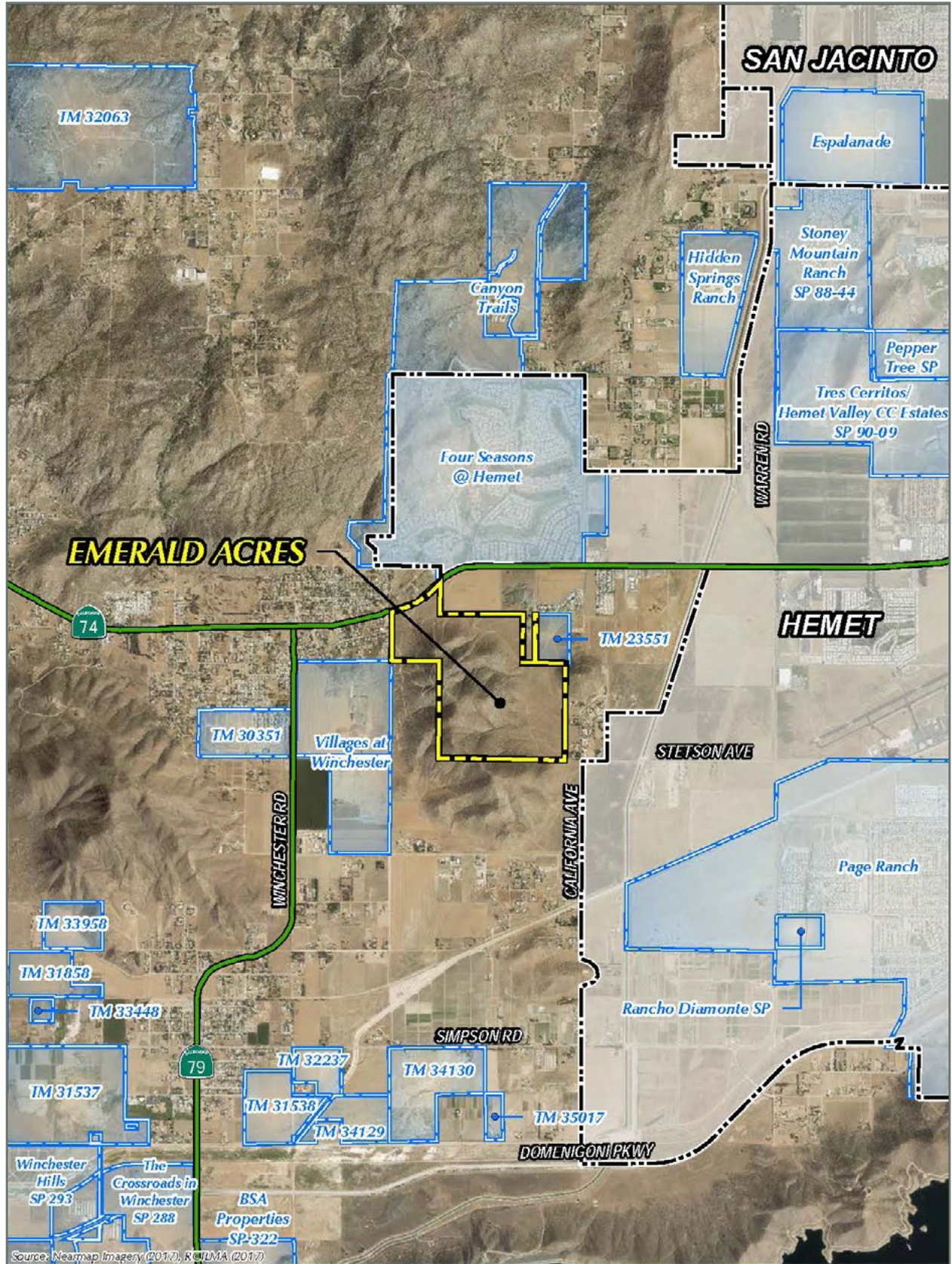


EXHIBIT 1-4. SURROUNDING DEVELOPMENT

Stormwater management within Emerald Acres utilizes a system of detention basins, drainage channels and water quality basins, to control stormwater flows and eliminate historic flooding of adjacent properties. Some of the detention basins may contain landscaping designed to provide opportunities for passive recreation.

1.2. PROJECT LOCATION AND SETTINGS

The Emerald Acres community is located within the Harvest Valley/Winchester area of unincorporated Riverside County, just west of the City of Hemet, and partially with the city's sphere of influence. The northernmost point of the Specific Plan area borders the City of Hemet and the eastern boundary of the site is approximately 0.5-mile west of the City of Hemet. The City of Menifee is located 4 miles west of the Project site and Diamond Valley Lake lies approximately 2.5 miles south of the site (See Exhibit 1-1, Regional Map). The Emerald Acres community is bordered by Florida Avenue to the north, Calvert Avenue to the west and Stetson Avenue to the south and is approximately 0.5-mile east of Winchester Road/Highway 79. Florida Avenue (State Highway 74), is a major County transportation corridor linking the Hemet Valley with Interstate 215 to the west, and is the location of one of the primary entries to Emerald Acres. In addition, the Hemet-Ryan Airport is located approximately one mile east of the Project (see Exhibit 1-2, Vicinity Map).

The community consists of portions of Section 14, Township 5 South, Range 2 West, East San Bernardino Base, and Meridian and can be located on the United States Geological Survey (USGS) Winchester 7.5 topographic quadrangle map.

1.3. SITE CHARACTERISTICS

1.3.1 LAND USE DESIGNATIONS

The Emerald Acres site is located within the County of Riverside's Harvest Valley/Winchester Area Plan with land uses currently designated as 10.5 acres of "Commercial Retail (CR)," 63.9 acres of "Medium-High Density Residential (MHDR)," 1.3 acres of "Medium Density Residential (MDR)," 77.4 acres of "Low Density Residential (LDR)," and 181.1 acres of "Rural Mountainous (RM)." The northern portion of the Emerald Acres site has historically been used for agricultural purposes, but under existing conditions is vacant and undeveloped. The southern and western portion of the Specific Plan contain rocky hills and boulders and remain vacant and undisturbed.

TABLE 1-2. EXISTING BUILD-OUT ASSUMPTIONS

Land Use	Acres	Midpoint Density	Dwelling Units
Commercial Retail (CR)	10.5 ac	0 du/ac	0 du
Medium-high Density Residential (MHDR)	63.9 ac	6.5 du/ac	415 du
Medium Density Residential (MDR)	1.3 ac	3.5 du/ac	4 du
Low Density Residential (LDR)	77.4 ac	1.5 du/ac	116 du
Rural Mountainous (RM)	181.1 ac	0.05 du/ac	9 du
Total	334.2 ac		544 du

Additionally, Emerald Acres is located within the County of Riverside's Highway 79 Policy Area. Harvest Valley/Winchester Area Plan Policy 7.2 requires that "development projects produce traffic generation at a level that is 9% less than the trips projected from the General Plan." The General Plan Socioeconomic Build-out Assumptions and Methodology anticipates 6.5 du/ac for MHDR, 3.5 du/ac for MDR, 1.5 du/ac for LDR, and 0.05 du/ac for RM land uses within the Highway 79 Policy Area. Table 1-2, Existing Build-

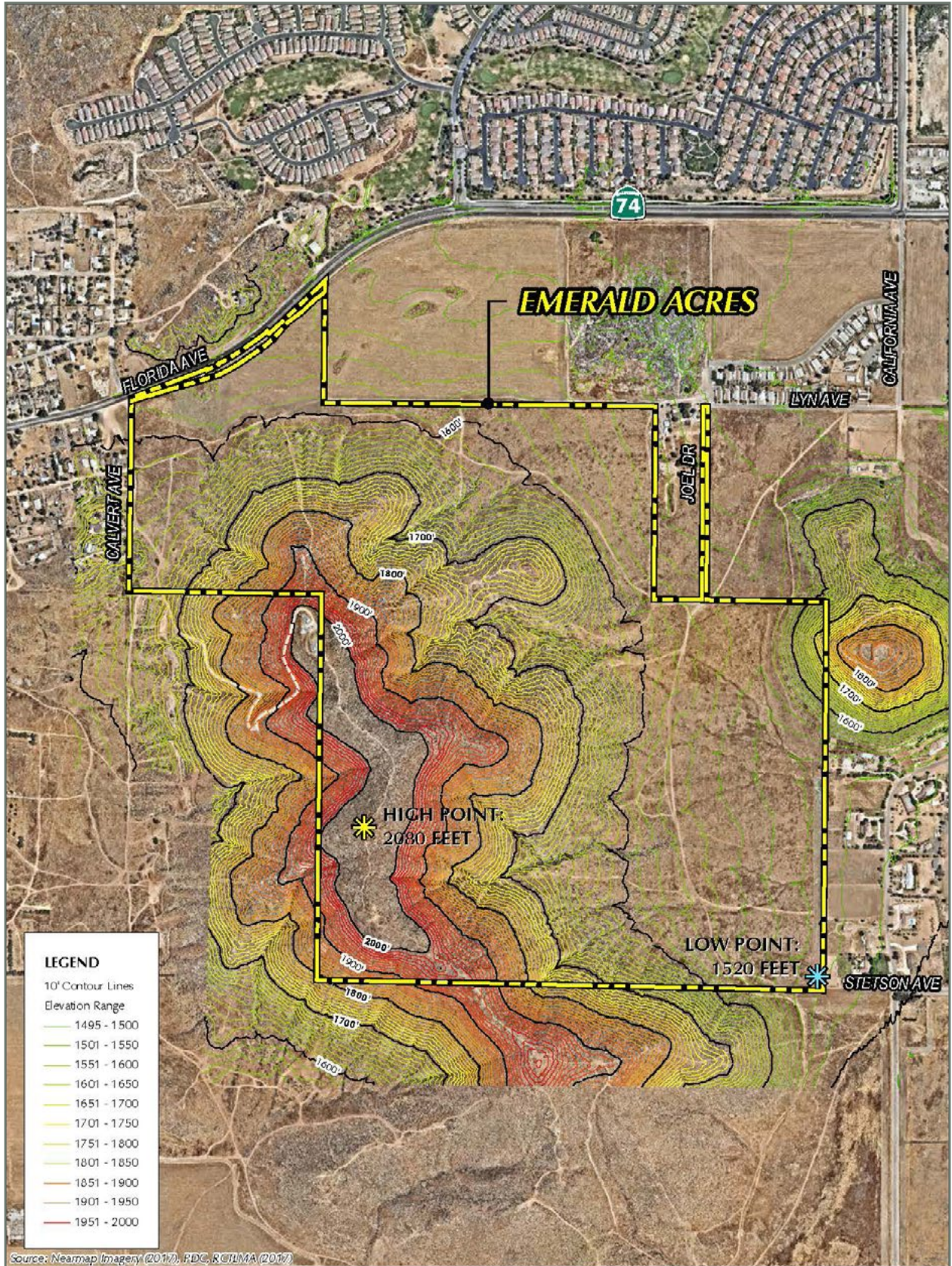


EXHIBIT 1-5. EXISTING TOPOGRAPHY

Out Assumptions, calculates that the General Plan anticipates 544 dwelling units on the Emerald Acres site under existing General Plan Land Use designations.

The 391 homes proposed in Emerald Acres is 28% less than those anticipated by the General Plan, and is therefore consistent with the 9% reduction required by the Highway 79 Policy Area policies.

1.3.2 SURROUNDING LAND USES AND DEVELOPMENT

The Emerald Acres Specific Plan is located within an area along Florida Avenue where rural residential uses transition to urban and suburban residential and commercial development. Exhibit 1-3, Aerial Photograph, and Exhibit 1-4, Surrounding Development Map, depict the Emerald Acres community and surrounding properties. Many of the properties immediately surrounding Emerald Acres are currently undeveloped, with those undeveloped properties adjacent to Florida Avenue being designated as residential and/or commercial by the County General Plan. However, there are existing single-family homes located along the west boundary of the community (south of Florida Avenue) in the adjacent rural community of Green Acres, and the community of Stetson Estates located at Stetson Avenue along the eastern site boundary. The Four Seasons community, a 665-acre master-planned golf course community with 1,775 housing units, is located north of the site, on the north side of Florida Avenue. Further to the east, located in the City of Hemet, is the Hemet-Ryan Airport.

1.3.3 PHYSICAL SITE CONDITIONS

The Emerald Acres community is characterized by a combination of steep hillside terrain, which will primarily remain in open space, and a gently sloping alluvial plain. The central and southwestern portion of the community is dominated by a north-south trending ridge line that drains to the east and west. Elevations within the site range from 1,520 feet within the alluvial plains to 2,080 feet along the ridge tops (see Exhibit 1-5, Existing Topography Map). Views of the Hemet Valley, the City of Hemet and the San Bernardino Mountains to the north and east exist from several locations within the site.

1.4. DOCUMENT PURPOSE

Authorized by California Government Code §65450 et seq., a Specific Plan is a tool that is used for the systematic implementation of the General Plan for all or part of the area covered by the General Plan. It effectively establishes a link between implementing policies of the General Plan and the individual development proposals in a defined area. As such, this document provides the County of Riverside with policies and regulations to ensure efficient, orderly development of the subject property in accordance with the County's adopted General Plan.

The Emerald Acres Specific Plan establishes standards for the development of a master planned community in the Harvest Valley/Winchester area of unincorporated Riverside County. This Specific Plan includes regulations relative to land uses, site planning, and building intensity, as well as design guidelines that are intended to allow innovative architecture, landscaping and building arrangements, as future tentative maps and site plans are proposed to implement the Specific Plan. All future implementing actions (development plans, tract maps, site plans, and other similar entitlements) for property located within the boundaries of this Specific Plan are required to be consistent with the standards and guidelines set forth in this document and with all applicable County regulations. Furthermore, all regulations, conditions, standards, and guidelines contained herein shall be deemed distinct and independent provisions of the Specific Plan. If any section, clause, phrase, or portion of this document is for any reason to be invalid by the decision of any federal or state court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this Specific Plan.

1.5. SPECIFIC PLAN FORMAT

This Emerald Acres Specific Plan has been prepared pursuant to the provisions of California Government Code §65450 et seq., which establishes the authority to adopt a Specific Plan, identifies the required contents of a Specific Plan, and mandates consistency with the General Plan. According to §65450 et seq., a Specific Plan shall include text and a diagram or diagrams which specify all of the following details:

- The distribution, location, and extent of the uses of land within the area covered within the specific plan area;
- The distribution, location, extent, and intensity of major circulation and utility services to be located within the plan area or that will be needed to service the specific plan area;
- Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable;
- A schematic program of implementation measures indicating how public services will be financed; and
- A statement of the specific plan's relationship to the general plan.

California state law also provides for the inclusion of any other subject that, in the judgment of the local planning agency, is deemed necessary or desirable to implement the general plan, such as architectural or landscape design guidelines.

This Emerald Acres Specific Plan is divided into the following sections: 1) Introduction; 2) Specific Plan; 3) Development Standards; 4) Design Guidelines; 5) Zoning Ordinance; and 6) General Plan Consistency. In response to government requirements, this Specific Plan has been prepared to provide the essential link to the policies of the County of Riverside General Plan. By functioning as a regulatory document, the Emerald Acres Specific Plan provides a means of implementing and detailing the County's General Plan and tailoring its policies to the subject property. In this regard, all future development plans or other entitlement applications are required to substantially conform to the standards and guidelines set forth in this document, as well as all applicable County regulations. This Specific Plan is designed to address site specific issues such as building setbacks and visual appearance, as well as community-wide concerns such as vehicular and non-vehicular circulation, energy conservation, landscaping, and the provision of utilities. The Emerald Acres Specific Plan also ensures that new development meets or exceeds County standards for environmental protection, infrastructure, site planning, and aesthetic quality.

1.6. DISCRETIONARY ACTIONS AND APPROVALS

The Riverside County Planning Department is the Lead Agency for the Emerald Acres Specific Plan, under whose authority this Specific Plan has been prepared. This document will be used by the County of Riverside in connection with the following decisions.

1. RIVERSIDE COUNTY PLANNING COMMISSION

- Recommendation to the Board of Supervisors regarding adoption of Specific Plan No. 381 by Resolution.
- Recommendation to the Board of Supervisors regarding approval of General Plan Amendment No. 1111 by Resolution.
- Recommendation to the Board of Supervisors regarding approval of Change of Zone No. 7774 by Ordinance.

- Recommendation to the Board of Supervisors regarding certification of EIR No. XXXX by Resolution.
- Approval of Tentative Tract Map No. 36542 by Resolution.
- Financial Map.

2. RIVERSIDE COUNTY BOARD OF SUPERVISORS

- Approval of Specific Plan No. 381 by Resolution.
- Approval of General Plan Amendment No. 1111 by Resolution.
- Approval of Change of Zone No. 7774 by Ordinance.
- Certification of EIR No. XXXX by Resolution.
- Receive and File Tentative Tract Map No. 36542.
- Financial Map.

Subsequent discretionary actions may include but are not limited to tentative and final tract maps, plot plans, grading permits, improvement plans, water and sewer systems, and encroachment permits.

1.7. PLANNING OBJECTIVES

To ensure the functional integrity, economic viability, environmental sensitivity and positive aesthetic impact of this Specific Plan, planning and development goals for the Project were established and supported through an extensive analysis. The analysis includes an examination of engineering feasibility, market acceptance, economic viability, County General Plan goals, and development phasing. Specifically, the following planning objectives for Emerald Acres assist the County of Riverside to:

- Provide for the establishment of a master-planned community, consisting of 391 homes along with a commercial component and recreational amenities, which is sensitive to the environment as well as aesthetically pleasing.
- Incorporate topographic, geologic, hydrologic, and environmental opportunities and constraints to create a design that compliments the condition of the land by maintaining and using basic landforms, where practical.
- Provide for high quality residential housing with public and private recreation amenities.
- Implement housing type diversity by providing both single and multi-family residential product types in a layout designed to take advantage of the sites' scenic resources that will be marketable within the evolving economic profile of western Riverside County.
- Establish an internal circulation system that meets Project needs and accommodates a variety of transportation modes.
- Provide facilities, including parks and open space to support development in an efficient and timely manner as well as meet the needs of community residents.
- Protect valuable scenic resources located on the Project site to preserve and enhance the character and identity of the community and region.
- Develop guidelines for architecture, landscaping, paving, walls, fencing, signage, and entry treatments that reinforce the community identity of the community and vicinity.

- Develop an environment that is visually attractive as well as efficiently and effectively organized, including a pleasing landscape palette.
- Integrate the community with the character of the surrounding community and establish development that results in logical coordinated growth.
- Provide a long-range comprehensive planning approach to development that cannot be accomplished on a parcel-by-parcel basis.
- Provide for neighborhood-serving commercial uses to allow for land use diversity and to provide convenient retail options for residents.

1.8. PLANNING APPROACH

The Emerald Acres Land Use Plan evolved from a project team composed of industry professionals from a variety of disciplines. The team created a comprehensive development plan for the 335.5 acres that is financially feasible, sensitive to the environment, and consistent with the County of Riverside General Plan and its intent for the area. These efforts result in the creation of a community that fulfills the residential, infrastructure, open space, and recreational needs of the community.

A series of opportunities and constraints analyses were utilized to determine areas most suitable for preservation and development, which included the following:

- Aerial Photography
- Adjacent Land Uses (existing and proposed)
- Topography (slopes, high points, low points, viewpoints, ridges, and valleys)
- Biology (plants and animals)
- Hydrology (surface water, groundwater, floodplains, and wetlands)
- Geology (physiographic, surficial geology, and geomorphology)
- Soils (stability, erodibility, and suitability)

While the opportunities and constraints process aided the team in defining the areas suitable for preservation, it also assisted in determining areas suitable for development. With the natural open space and drainage system defined and a development envelope in place, the project team designed an integrated plan for infrastructure to serve the needs of the Emerald Acres community.

The roadway system and natural features define approximate boundaries for several of the Planning Areas, which were established according to logical placement, lot size, project phasing and surrounding land uses. Planning Areas consist of a variety of residential neighborhoods, retail facilities, open space, and recreational uses. The resulting Emerald Acres Land Use Plan provides for a community that enhances opportunities for open space preservation, recreation, and housing variety.

SECTION 2: SPECIFIC PLAN

2.1. LAND USE PLAN

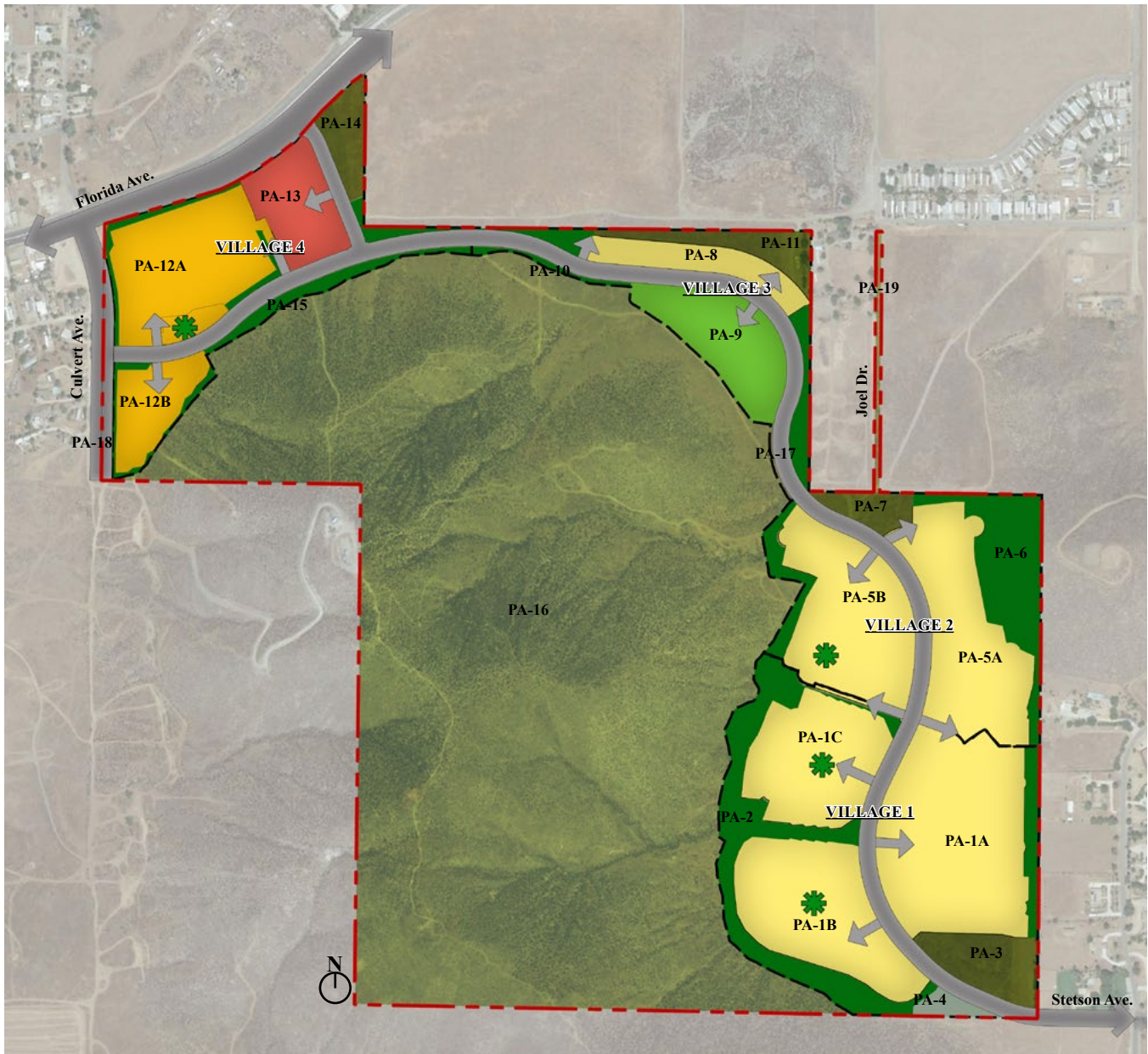
2.1.1 DEVELOPMENT CONCEPT

Expansive views of the Hemet Valley and the mountains beyond, high-quality design, and a wide array of amenities define the spirit of Emerald Acres, a 335.5-acre master-planned community in the Harvest Valley area of unincorporated Riverside County. The Emerald Acres Specific Plan is designed as four distinct residential villages served by a commercial planning area, a public Neighborhood Park, village recreation areas, and a Multi-Use Trail easement. This community of villages is a unique and inviting place. Sidewalks and trails within landscaped paseos and parkways encourage and enhance pedestrian and non-vehicular activity between the various villages within Emerald Acres. Residential product types and densities meet the housing needs within the western Riverside County, while creating a community which preserves the steep slopes and natural open space of the site.

The design of Emerald Acres is in direct response to the existing physical attributes of the site and existing adjacent communities, taking advantage of the adjacent natural open space and hillside setting. Many of the homes will have sweeping views of the Hemet Valley and City of Hemet to the east, the San Bernardino Mountains to the north and east, as well as views of the Lakeview Mountains, which form the site's southwestern boundary. Rock outcroppings, sloping terrain, and generous expanses of natural open space define and enhance the community's identity. Thematic elements such as walls, fencing, landscaping, streetscapes, and monumentation are utilized to promote the community image and are described in Section 4, Design Guidelines.

Emerald Acres provides for the development of a maximum of 391 homes including both detached and attached housing styles (see Exhibit 2-1, Land Use Plan). To provide a wide range of housing opportunities and create unique residential experience, the Emerald Acres community is divided into four (4) residential villages and one (1) commercial planning area with densities ranging from 4.0 to 9.1 dwelling units per acre, with an overall density of 1.2 dwelling units per acre. Single-family residential uses are provided in Villages 1, 2, and 3. Village 4 is envisioned as a high density, residential neighborhood which can be developed with a variety of product types, including, but not limited to attached and detached motorcourt homes. Planning Area 13 in Village 4, adjacent to Florida Avenue, provides residents with neighborhood commercial uses and convenient access to an important regional transportation corridor.

As summarized in Table 2-1, Detailed Land Use Summary, the Specific Plan Land Use Plan is organized into four (4) residential villages with one (1) commercial planning area, as well as a series of planning areas that provide for open space, recreation, and public facility uses. Specific information on each individual village within the Emerald Acres Specific Plan is provided in Section 3, Planning Area Development Standards.



LEGEND

- Medium Density Residential (6,000 sf Min. Lots)
- Medium Density Residential (5,000 sf Min. Lots)
- High Density Residential (Attached/Detached Motorcourt)
- Commercial Retail

- Open Space-Recreation (Neighborhood Park)
- Open Space-Conservation (Slopes)
- Open Space-Conservation (WQMP Basin)
- Open Space-Conservation Habitat (Natural Open Space)

- Public Facilities
- Backbone Road
- Village Recreation Area
- Project Boundary

EXHIBIT 2-1. LAND USE PLAN

TABLE 2-1. DETAILED LAND USE SUMMARY

Planning Area	Village	Land Use	Acres	Density Range	Target Density	Target Units
1A, 1B, 1C	Village 1	Medium Density Residential 6,000 s.f. Min. Lots	35.8	2-5 du/ac ¹	4.0 du/ac	144
2		Open Space - Conservation Slopes	10.6			
3		Open Space - Conservation WQMP Basin	3.7			
4		Public Facilities	0.8			
5A, 5B	Village 2	Medium Density Residential 5,000 s.f. Min. Lots	22.5	2-5 du/ac	4.7 du/ac	105
6		Open Space - Conservation Slopes	7.8			
7		Open Space - Conservation WQMP Basin	1.7			
8	Village 3	Medium Density Residential 5,000 s.f. Min. Lots	3.7	2-5 du/ac	5.0 du/ac	19
9		Open Space - Recreation Neighborhood Park	6.6			
10		Open Space - Conservation Slopes	2.8			
11		Open Space - Conservation WQMP Basin	2.8			
12A, 12B	Village 4	High Density Residential Attached or Detached Motorcourt	13.5	8-14 du/ac	9.1 du/ac	123
13		Commercial Retail	5.0			
14		Open Space - Conservation WQMP Basin	2.8			
15		Open Space - Conservation Slopes	6.8			
16		Open Space - Conservation Habitat Natural Open Space	191.3			
17		Backbone Roads	15.4			
18		Existing Road Easement	1.3			
19		Joel Drive Extension	0.9			
TOTAL		PROJECT TOTAL	335.5	-	1.2 du/ac	391

2.1.1.1 RESIDENTIAL

Residential Villages account for approximately 75.2 acres of the Emerald Acres community and contain a maximum of 391 dwelling units, for a net residential density of 5.2 dwelling units per acre and a gross Specific Plan density of 1.2 dwelling units per acre. In conformance with Specific Plan goals, a variety of housing types, lot sizes, lot configurations, and densities are provided. This variation is intended to create housing opportunities for Emerald Acres residents in various economic circumstances.

- Medium Density Residential (2-5 du/ac): Emerald Acres includes a maximum of 268 Medium Density Residential homes distributed between Village 1, Village 2, and Village 3. Village 1 contains a maximum of 144 single-family detached homes on minimum 6,000 s.f. lots. Village 2 contains a maximum of 105 single-family detached homes on minimum 5,000 s.f. lots. Village 3 contains a maximum of 19 single-family detached homes on minimum 5,000 s.f. lots.
- High Density Residential (8-14 du/ac): The Emerald Acres Specific Plan provides for the development of Village 4 with a maximum of 123 High Density Residential homes, developed as attached or detached motorcourt residences.

2.1.1.2 OPEN SPACE AND RECREATION

- Neighborhood Park: Planning Area 9 in Village 3, located in the southeastern portion of the community, includes a 6.6-acre Public Park Area which provides recreational facilities within the neighborhood and serves as the focal point of activity for the Emerald Acres community. The park may include a basketball court, baseball diamond, soccer field, picnic grove, tot lot, restroom facilities, and on-site parking.
- Open Space – Conservation: The Emerald Acres community includes open space areas, totaling 39.0 acres within and near residential villages. These open space areas accommodate stormwater percolation basins, manufactured slope areas, and Fire Fuel Modification Zones. Planning Area 2 in Village 1, an Open Space – Conservation area, also includes an equestrian trail access point.
- Open Space – Conservation Habitat: Planning Area 16, a total of 191.3 acres of natural open space is provided within Emerald Acres for conservation of the local landscape and habitat. The majority of the community’s open space acreage, which includes most areas with slopes of 25% or greater, has been left natural and undeveloped to preserve the backdrop of the Lakeview Mountains, scenic topographic features, and rock outcroppings found in these areas. Although erosion control and fuel modification measures will be applied to portions of undeveloped slope areas adjacent to major roadways and residential planning areas within the community, these disturbed areas will be re-vegetated and owned and maintained by the Master Homeowners’ Association.
- Detention Basins: A storm water detention basin that protects the adjacent Stetson Estates community is located at the southeastern corner of the Emerald Acres community, within Planning Area 3. This basin is sized to accommodate surface storm water flows in the southern portion of Emerald Acres, and is landscaped around its perimeter to provide a buffer between this basin and between the Emerald Acres community from the adjacent Stetson Estates community to the east. Stormwater flows are directed to this by a channel system linking the basin with the undeveloped hillside terrain to the west. Three additional detention basins are located within Planning Areas 7, 11 and 14. In total, there are 10.6 acres of detention basins within the community.

2.1.1.3 COMMERCIAL RETAIL

Residents of Emerald Acres, as well as travelers on Florida Avenue, will have 5.0-acre of convenient commercial uses provided by Planning Area 13 in Village 4. The commercial area is accessible via Street E and a shared access driveway from Street A, located in between Planning Area 12 and 13 in Village 4. The Multi-Use Trail located along Street A provides a pedestrian connection to Planning Area 13 for residents throughout the Emerald Acres community.

2.1.1.4 ROADS

The Emerald Acres Specific Plan provides for the construction of approximately 15.4 acres of backbone roadways onsite, including Street A, and the entry roads linking the community to Florida Avenue and Stetson Avenue. Within the Emerald Acres community, local traffic is accommodated by a hierarchical circulation system of public and private roads, with rights-of-way ranging from 37 to 85 feet in width.

2.1.2 LAND USE DEVELOPMENT STANDARDS

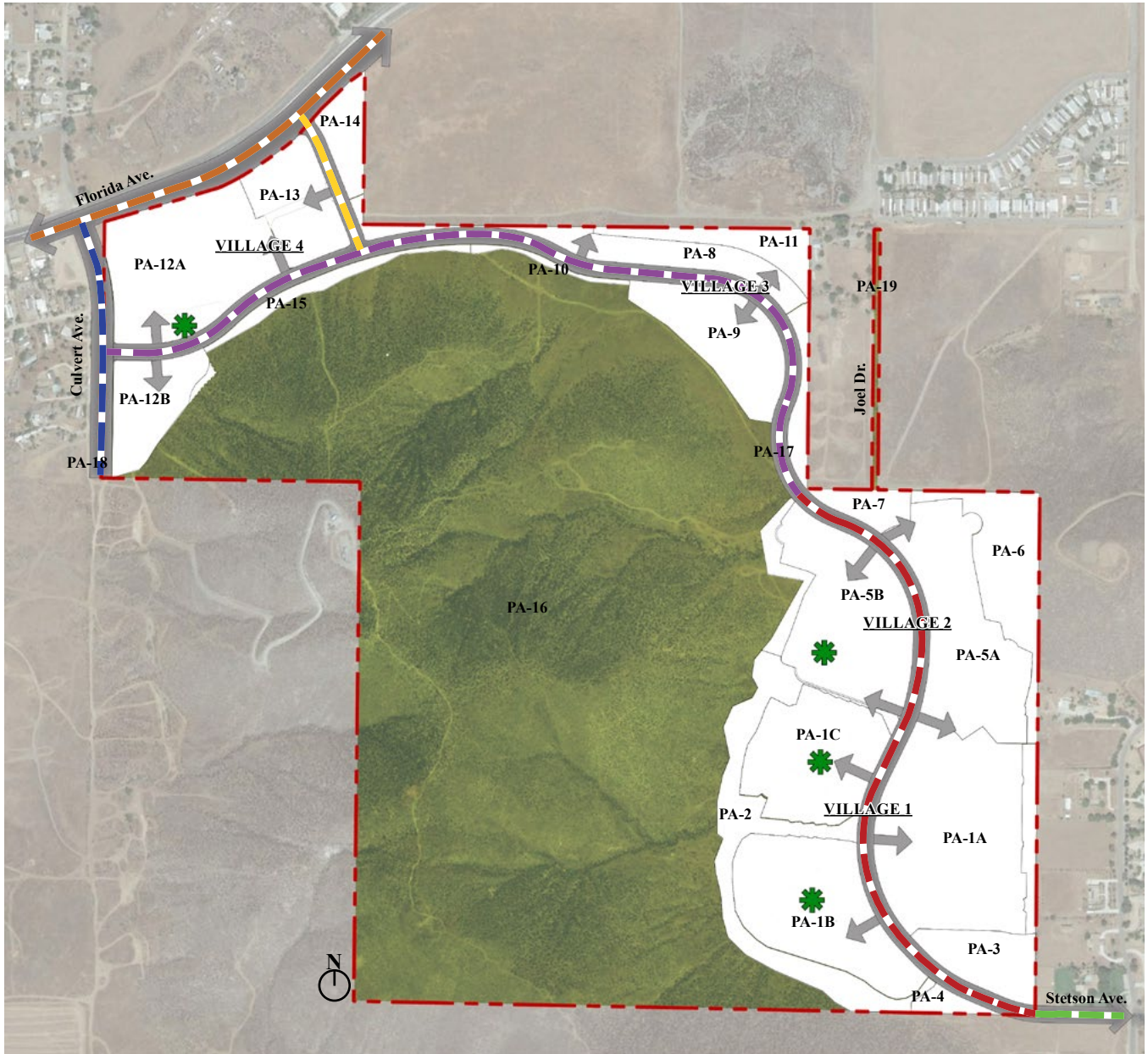
To ensure the orderly and sensitive development of the Emerald Acres Specific Plan, development standards have been prepared for each Village to facilitate the efficient implementation of planned development (refer to Section 3, Development Standards). In addition, community-wide development standards also have been prepared to complement the unique conditions within each Planning Area. The community-wide standards are as follows:

1. The Emerald Acres Specific Plan area shall be developed as a master planned community containing Medium Density Residential, High Density Residential, Commercial Retail, Open Space–Recreation, Open Space–Conservation, Open Space–Conservation Habitat, Public Facilities, and Circulation land uses on 335.5 acres, as illustrated on Exhibit 2-1, Land Use Plan, and in the individual Village exhibits (Exhibits 3-1 through 3-13).
2. The area designated as Emerald Acres Specific Plan No. 381 shall be developed with a maximum of 391 dwelling units. Table 2-1, Detailed Land Use Summary, provides the maximum dwelling units permitted within each Planning Area.
3. The proposed number of dwelling units contained in an implementing subdivision application may be more or less than the totals specified in Table 2-1, Detailed Land Use Summary, without necessitating the preparation of a Specific Plan Amendment or Substantial Conformance provided that the proposed density does not exceed the designated target density for each Village area and the maximum number of dwelling units within the Specific Plan area does not exceed 391 units. Should the proposed density or dwelling unit count within a subdivision application be outside of the Specific Plan Density Range of a particular Village, or outside the total number of units for the entire Specific Plan, the County of Riverside may require the approval of a Specific Plan Amendment, Substantial Conformance, or as approved by the Planning Director.
4. Land use and development standards shall be in accordance with the Emerald Acres Zoning Ordinance and will be further defined by the Emerald Acres Specific Plan objectives, the Emerald Acres Specific Plan Design Guidelines, and future detailed development proposals including tentative maps, site plan, site development permit, and/or conditional use permits. When appropriate and necessary to meet the goals of this Specific Plan, the standards contained within Emerald Acres Specific Plan shall take precedence over the requirements within Riverside County Ordinance No. 348. However, where the Emerald Acres Specific Plan is silent, the land use and development standards contained within Riverside County Ordinance No. 348 shall generally, but not exclusively, prevail.
5. Standards regarding landscaping, signage, and other related design elements shall conform to the

design standards contained within this Specific Plan, Riverside County Ordinance No. 348, and any other applicable Riverside County ordinances. When appropriate and necessary to meet the goals of this Specific Plan, the standards contained within Emerald Acres Specific Plan shall take precedence over the requirements within Riverside County Ordinance No. 348.

6. Development of the property within the Emerald Acres Specific Plan area shall be in accordance with the mandatory requirements of all Riverside County ordinances, including Ordinance Nos. 348 and 460, except where this Specific Plan states otherwise.
7. Except for the Specific Plan Zoning Ordinance adopted concurrently with this Specific Plan, no portion of the Specific Plan, which purports or proposes to change, waive, or modify any ordinance or other legal requirement for the development, shall be considered to be part of the adopted Specific Plan.
8. A review in compliance with the California Environmental Quality Act (CEQA) shall be conducted to determine potential environmental impacts resulting from each tract, change of zone, plot plan, specific plan amendment, or any discretionary land use application required to implement the Specific Plan, unless said proposal is determined to be consistent with EIR No. XXXX and does not require subsequent environmental documentation, or is exempt from the provisions of CEQA. Any required CEQA review shall be prepared as part of the review process for these implementing projects.
9. Planning Areas designated by this Specific Plan and parcels created pursuant to any implementing tentative parcel or tentative tract maps shall be in conformance with the development standards of the Specific Plan zone herein applied to the property.
10. A 15% variation in Planning Area acreage may be allowed without a Specific Plan Amendment. Any acreage change in a Planning Area of over 15% may be approved by the Planning Director or will require a Specific Plan Amendment. However, a Specific Plan Amendment is required for any change to Planning Area acreage that results in the Planning Area's density being below its assigned density range.
11. Prior to issuance of a building permit for construction of any use contemplated by this Specific Plan approval, the applicant shall first obtain clearance from the County of Riverside Planning Department verifying that all pertinent conditions of Specific Plan approval have been satisfied for the phase of development in question.
12. Common areas identified in the Specific Plan shall be owned and maintained as follows:
 - A permanent master maintenance organization shall be established for the Specific Plan area to assume ownership and maintenance responsibility for all common open space, circulation systems, trails, and landscaped areas that are not under the maintenance responsibility of the County of Riverside, or another public or quasi-public organization.
 - Unless otherwise provided for in these standards, common areas shall be conveyed to the maintenance organization as implementing development is approved or a conveyance subdivision is recorded.
 - The ownership and maintenance responsibility shall be identified for each open space lot at the time subdivision maps are filed. The maintenance organization may be established prior to, or concurrent with, approval of the first tract map or issuance of any building permit for any approved development permit.
13. Development applications which incorporate common areas shall be accompanied by design plans for the common areas, specifying location and extent of landscaping, irrigation systems, structures, and circulation (vehicular, pedestrian and/or equestrian) or approval shall be conditioned upon provisions of those plans at the appropriate time.

14. All lighting within the boundaries of the Specific Plan shall be in accordance with applicable Riverside County standards. All lighting within the boundaries of the Specific Plan shall also be in accordance with the lighting standards set forth in Section 4, Design Guidelines, of this Specific Plan.
15. Construction of certain public facilities and infrastructure requirements (such as water lines, roadways, etc.), as well as payment of County fees, may be financed through an assessment district (AD) or a community facilities district (CFD). If facilities are financed through a CFD, then additional fees to finance those facilities shall not be required.
16. Prior to the issuance of building permits, improvement and irrigation plans for adjacent common areas shall be submitted for Planning Department approval. Irrigation plans shall be certified by a landscape architect.
17. In accordance with the California Solid Waste Reuse and Recycling Access Act of 1991, Cal Public Resources Code §42911, adequate areas for collecting and loading recyclable materials shall be provided.
18. For the security and safety of future residents, the developer shall incorporate the following design concepts within each individual tract:
 - Circulation for pedestrians, vehicles, and police patrols.
 - Lighting of streets and walkways.
 - Visibility of doors and windows from the street and between buildings, where practical.
 - Fencing heights and materials which are the developer's responsibility.
19. The following crime prevention measures shall be considered during site and building layout design, in addition to those above, for the security and safety of future residents:
 - Automatically lit addresses.
 - Special lighting requirements on any buildings that are grouped in such a way that individual addresses are difficult to read.
 - Front doors into residences that front toward or are visible from the street and allow for easy drive-by surveillance by law enforcement personnel, where practical.
20. Development within the community shall conform to Title 24, Chapter 2-71, of the California Administrative Code to ensure accessibility to physically challenged individuals.
21. Residential structures shall be constructed to comply with California Energy Commission Title 24, Energy Efficiency Standards for Residential Construction.
22. All landscaping shall be drought tolerant and in accordance with Riverside County Ordinance No. 859.
23. The Master and village property owners' association shall be charged with the unqualified right to assess their own individual owners who own individual units for reasonable maintenance and management costs which shall be established and continuously maintained. The Master Homeowners' Associations may be responsible for parking, open space areas, signing, landscaping, irrigation, common areas, and other responsibilities as necessary.
24. All water mains and fire hydrants providing required fire flows shall be constructed in accordance with the appropriate sections of Riverside County Ordinance No. 460 and/or No. 787, subject to approval by the Riverside County Fire Department.



LEGEND







- | | |
|---|---|
|  Florida Avenue - Modified Expressway (184' R.O.W.) |  Street A -Modified Collector (74' R.O.W.) |
|  Calvert Avenue - Modified Secondary Highway (112' R.O.W.) |  Street E -Modified Collector (74' R.O.W.-Private) |
|  Street A -Modified Collector (85' R.O.W.) |  Stetson Ave - Modified Local Street (63' R.O.W.) |

EXHIBIT 2-2. VEHICULAR CIRCULATION PLAN

2.2. CIRCULATION PLAN

2.2.1 VEHICULAR CIRCULATION PLAN DESCRIPTION

A Specific Plan roadway concept has been developed for the Emerald Acres Specific Plan, as illustrated on Exhibit 2-2, Vehicular Circulation Plan. The main objective of the Circulation Plan is to provide direct and convenient access to individual residential villages and other land uses through a safe and efficient network of private roadways and drives.

At build out, primary access to the community from Florida Avenue is provided via Street E, a private Modified Collector Road (74' right-of-way) with a 15' median. In addition, primary access to the community from Calvert Avenue is provided via Street A, a Modified Collector Road (74' right-of-way).

South of the boundary between Village 2 and 3, Street A widens to an 85' right-of-way Modified Collector Road and continues through the community, transitioning into Stetson Avenue at the southeastern edge of the community. The Specific Plan accommodates an off-site future road (un-named) that will be constructed by others, which will connect Street A to Florida Avenue through other ownerships to the north.

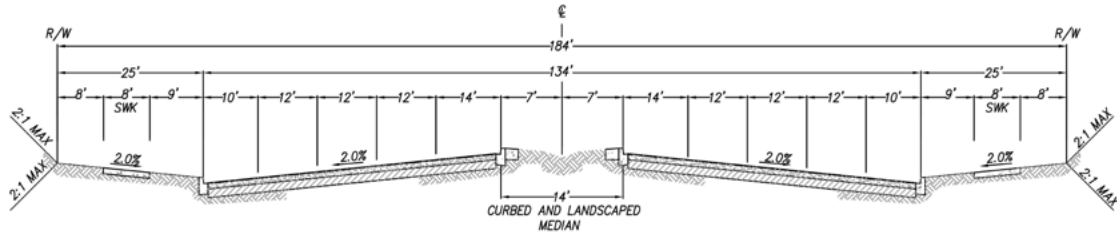
Roadway classifications within the Emerald Acres Specific Plan Circulation Plan have been designated in accordance with the County of Riverside General Plan Circulation Element. The following is a description of each of the roads as depicted on Exhibit 2-2. Specific Plan roadways are depicted in Exhibits 2-3 and 2-4, Roadway Cross Sections. Streetscape and roundabout designs for these roadways are provided in Section 4, Design Guidelines.

- Florida Avenue: Florida Avenue is designated as a Modified Expressway, with a 184' right-of-way. Florida Avenue provides regional access to the community, linking with Winchester Road and Interstate 215 to the west, and the City of Hemet to the east.
- Calvert Avenue: Calvert Avenue is designated as a Modified Secondary Highway, with a 112' right-of-way. Calvert Avenue provides regional access to the community Via Street A. Calvert Avenue also provides emergency vehicle access to Village 4B, located in the northwestern corner of the community, south of Street A.
- Street A: Street A is designated as a Modified Collector Road with a 74' right-of-way between Calvert Avenue and Village 3. This roadway is asymmetrically designed to accommodate a 10'-wide Multi-Use Trail that runs parallel to Street A along the northbound side of the roadway.

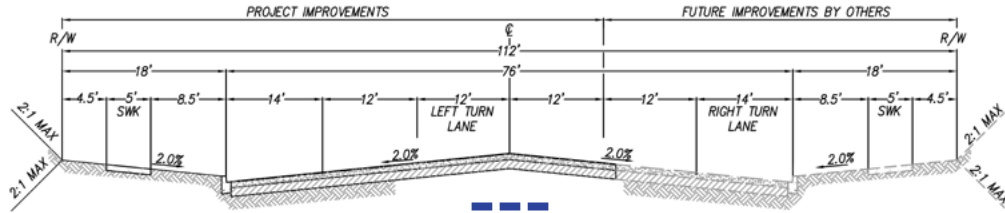
The southbound side of Street A between Calvert Avenue and Village 3 includes a 4' parkway, and northbound side of the road includes a 5' sidewalk and 4' sections of parkway in addition to the Multi-Use Trail. South of Village 3, Street A widens to an 85' right-of-way Modified Collector.

Road which includes the addition of a 5' sidewalk and 6' of additional parkway along the southbound side of the road. At the southeastern entry point of the community, Street A transitions into Stetson Avenue.

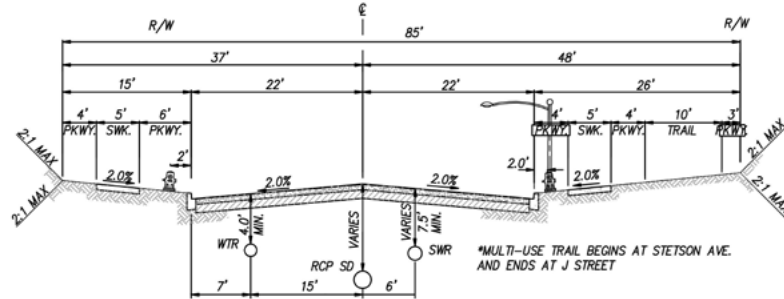
- Stetson Avenue: Stetson Avenue is designated as a Modified Local Street with a 63' right-of-way. Stetson Avenue provides the Emerald Acres community with regional access to the south via California Avenue. As Street A transitions to Stetson Avenue, lane widths decrease from 22' in each direction along Street A to 20' along Stetson Avenue. Additionally, southbound sidewalks reduce from 5' along Street A to 4.5', and northbound sidewalks increase to 5.5' adjacent to Stetson Avenue. Sidewalks along Stetson Avenue are flanked by 5.5' (southbound side) and 7.5' (northbound side) of parkway.



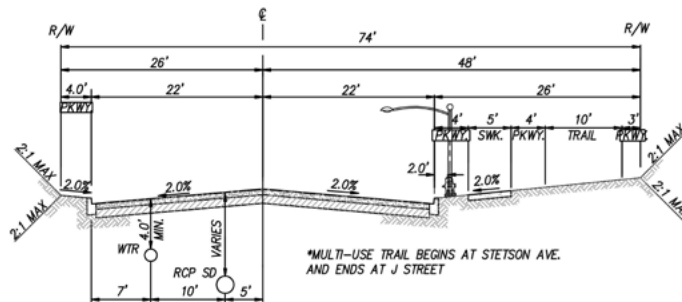
Florida Avenue - Modified Expressway
(184' R.O.W.) Public



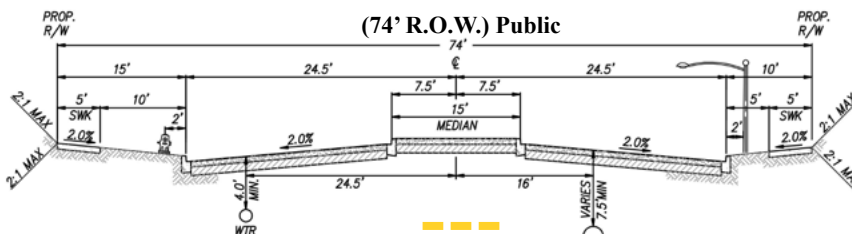
Calvert Avenue - Modified Secondary Highway
(112' R.O.W.) Public



Street A - Modified Collector
(85' R.O.W.) Public

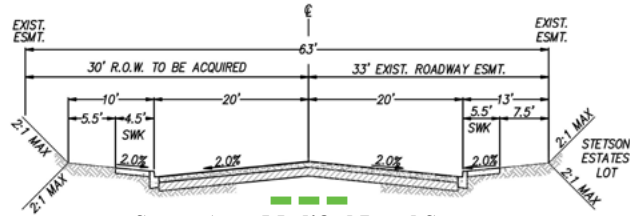


Street A - Modified Collector
(74' R.O.W.) Public

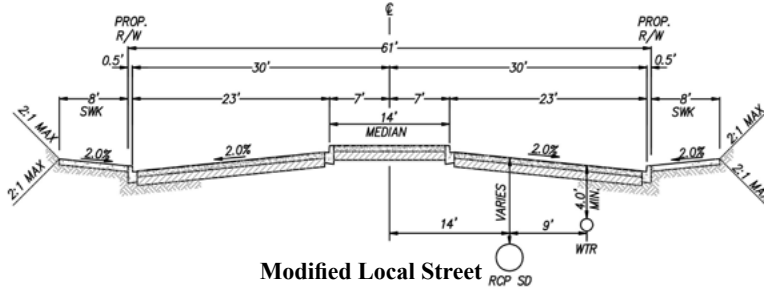


Street E - Modified Collector
(74' R.O.W.) Private

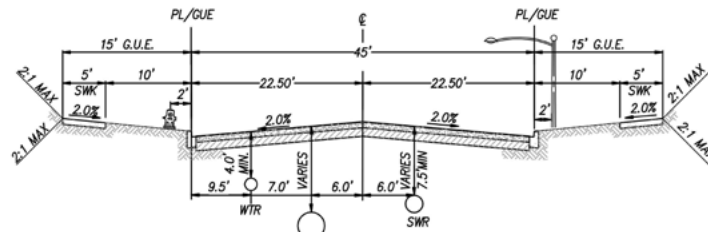
EXHIBIT 2-3. ROADWAY CROSS SECTIONS



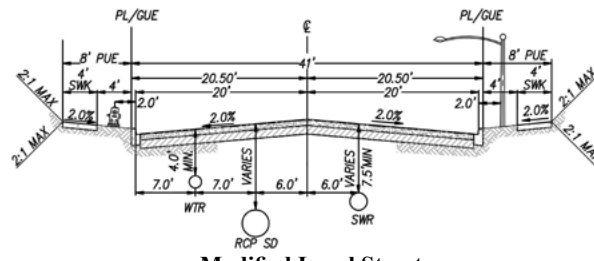
Steton Ave - Modified Local Street
 (63' R.O.W.) Public



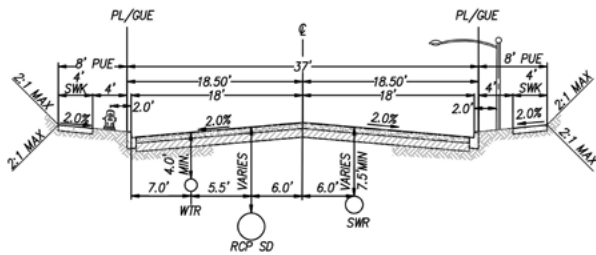
Modified Local Street
 (61' R.O.W.) Private



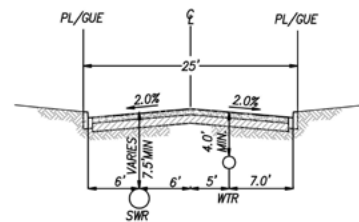
Modified Local Street
 (75' R.O.W.) Private



Modified Local Street
 (41' R.O.W.) Private



Local Street
 (37' R.O.W.) Private



Alley
 (25' R.O.W.) Private

EXHIBIT 2-4. ROADWAY CROSS SECTIONS

- Local Roads: Each local road in the Emerald Acres Specific Plan is designated as a Private Local Road, with right-of-ways ranging from 37 feet to 61 feet wide. Private Local Roads are roads that provide accessibility and connectivity within Planning Areas and are maintained by the Master Homeowner's Association (HOA). Local roads in Emerald Acres provide direct access to residential lots within each village.

2.2.2 CIRCULATION PLAN DEVELOPMENT STANDARDS

1. The proposed Circulation Plan provides a traffic design that meets the needs of the community. Roadway improvements are depicted on Exhibit 2-2, Vehicular Circulation Plan.
2. Off-site road will be constructed to half-width standards as follows:
 - Calvert Avenue – Modified Secondary Highway (112' right-of-way)
3. Off-site road will be constructed to full-width standards as follows:
 - Stetson Avenue – Modified Local Street (63' right-of-way)
4. On-site roads will be constructed to full width standards as follows:
 - Street A – Modified Collector (74' right-of-way, from Village 3 to Calvert Avenue)
 - Street A – Modified Collector (85' right-of-way, from Stetson Avenue to Village 3)
 - Private Modified Local Roads (37'-61' roadway)
 - Private Alleys (25' alleyway)
5. Public roads within the Specific Plan project boundary shall be constructed to appropriate County full or half-width standards in accordance with Ordinance Nos. 460 and 461 as a requirement of the implementing subdivisions for the Specific Plan or with the proposed modified sections subject to approval by the Director of Transportation.
6. Driveways and/or access points to Planning Areas from public streets as shown in the Emerald Acres Specific Plan are approximate locations and are subject to modification through County approval of implementing subdivision maps.
7. All intersection spacing and/or access openings shall be per Standard 114, Ordinance 461, or as approved by the County Transportation Department.
8. Mid-block crosswalks are prohibited.
9. Major roadway improvements may seek financing through an assessment district, community facilities district bond sales, and/or TUMF, or similar financing mechanism.
10. Landscape requirements shall be in accordance with the streetscape treatments depicted in Section 4, Design Guidelines.
11. Any landscaping within public road rights-of-way will require approval by the County Transportation Department and County Planning Department and assurance of continuing maintenance through the establishment of a landscape maintenance district or similar mechanism, as approved by the Transportation Department and Planning Department.
12. The project and subsequent implementing projects shall comply with the conditions and requirements set forth by the County Transportation Department.



LEGEND

- WQMP Basin
- Debris Basin
- Flow Direction
- Point of Connection
- Proposed Storm Drain Line-Offsite
- Proposed Storm Drain Line-Onsite
- Reinforced Concrete Box Culvert (5' X 4')

EXHIBIT 2-5. MASTER DRAINAGE PLAN

2.3. DRAINAGE PLAN

The Emerald Acres Specific Plan is situated at the southeast corner of the intersection of Calvert Avenue and Florida Avenue. It is bounded on the north, south, and east by undeveloped land, on the west by a subdivision near the northern terminus of Street E and by the Stetson Estates community along the southeast. The existing property area within Emerald Acres consists of open space natural hillsides with slopes ranging from 4% to 40%, with undeveloped land with natural ground cover.

2.3.1 DRAINAGE PLAN DESCRIPTION

The conceptual drainage system for the Emerald Acres community is illustrated on Exhibit 2-5, Master Drainage Plan. As shown on Exhibit 2-5, a network of drainage lines along with debris and Water Quality Management basins are provided throughout the Emerald Acres community to attenuate storm water runoff flows. The proposed debris and Water Quality Management basins are designed according the County of Riverside standards to detain and slowly release storm water to allow particles and associated pollutants to settle out.

The following is a summary of the Emerald Acres drainage plan:

- Village 1: Storm water that collects from the hillsides west of Village 1 travel easterly towards the two (2) debris basins within Planning Area 2. These debris basins naturally remove large debris products, common in storm water descending from steep slopes, and convey the storm water in a southeasterly direction towards the Water Quality Management and Protection (WQMP) basin within PA 3 through a series of storm water pipes. Storm water that collects within Village 1, east of Street A, gets directed into local storm drain catchments that terminate at the WQMP basin within PA 3. The WQMP basin utilizes natural percolation techniques to cleanse the runoff from Emerald Acres prior to entering the 72-inch storm drain located off-site to the southeast of the Project boundary.
- Village 2: Storm water from the hillsides west of Village 2 is collected and conveyed in easterly direction towards the WQMP basin within PA 7. Likewise, storm water that collects within Village 2, east of Street A, gets directed into local storm drain catchments that terminate at the WQMP basin within PA 7. After percolating through the WQMP basin, the storm water is directed off-site to the north of PA 7 within Joel Drive in a 54-inch storm drain that terminates at the existing 72-inch drainage line within Lyn Avenue. Storm water from the hillsides east of Village 2 are collected and conveyed in a southerly direction towards the WQMP basin within PA 3.
- Village 3: Storm water from the hillsides southwest of Village 3 is collected and conveyed toward the debris basin located within PA 10. Storm water that collects within Village 3 is collected into local storm drain catchments that also terminate at the WQMP basin within PA 11. After percolating through the WQMP basin, storm water is directed off-site to the east of Village 3 through a 60-inch storm drain that terminates at the existing 72-inch drainage line within Lyn Avenue at the intersection with Joel Drive.
- Village 4 and Planning Area 5: Storm water from the hillsides southeast of Village 4 is collected and conveyed toward the debris basin within PA 15. From there, storm water is conveyed in a northeasterly direction towards the WQMP basin within PA 14. The basin within PA 14 also collects the storm water from the commercial area (PA 13) that is located directly west of the basin. After percolating through the WQMP basin, the storm water is directed off-site and outlets northeast of PA 14, along Florida Avenue.
- Offsite Drainage Improvements: Off-site drainage improvements include the installation of a 72-inch drainage line that connects at the intersection of Street A and California Avenue, a 60-inch storm drain line within California Avenue that outlets to the San Diego canal southeast of the Emerald Acres

community (approximately 0.50-mile from the site), and a 60-inch Storm Drain to the east of Village 3 that connects to Lyn Avenue.

2.3.2 DRAINAGE PLAN DEVELOPMENT STANDARDS

1. The local on-site storm drain facilities shall be maintained by the Homeowners Association and shall be designed in accordance with standards and the requirements of the Riverside County Flood Control and Water Conservation District and the Riverside County Transportation Department.
2. Grading and drainage improvements shall conform to the California Building Code. Grading of the site shall be designed so as to protect all building pads from the 100-year storm event and to convey offsite runoff safely through the site.
3. Subsurface storm drains within the public right-of-way shall be reinforced concrete pipe. Subsurface storm drain facilities within the private property may utilize an approved material alternative such as High-Density Polyethylene (HDPE).
4. A Notice of Intent will be submitted to the Regional Water Quality Control Board and a site specific Stormwater Pollution Prevention Plan/Water Quality Management Plan will be prepared prior to the commencement of construction and post-development activities. The permits will require the applicant to implement source control and structural best management practices during and after construction activities. The County will be responsible for implementation of the best management practices.
5. The Specific Plan shall comply with the requirements of the Santa Ana Regional Water Quality Control Board (SARWQCB) Order No. R8-2002-0011, which is applicable to developments located within the Santa Ana watershed.

2.4. WATER AND SEWER PLAN

2.4.1 WATER PLAN DESCRIPTION

The Emerald Acres Specific Plan area is within the jurisdictional limits of the Eastern Municipal Water District (EMWD), within EMWD 1811 Pressure Zone. On-site facilities are sized in accordance with EMWD criteria to meet the appropriate domestic and fire flow requirements for the Emerald Acres land uses. Although not anticipated, it may be necessary to relocate, upsize, or update some portions of the existing waterline system to bring the system into full compliance with current standards.

2.4.1.1 POINTS OF CONNECTION

Emerald Acres is serviced by a network of existing water facilities, which includes a 12-inch line in Stetson Avenue, and a 12-inch line in Florida Avenue. Exhibit 2-6, Master Water Plan, shows the location and approximate sizes of on-site and off-site water facilities.

As shown on Exhibit 2-6, the water main within Florida Avenue connects the northwest portion of the Emerald Acres site to the EMWD network. Water is conveyed into the Emerald Acres site from the intersections of Florida Avenue and Street E and Florida Avenue and Calvert Avenue, and at a connection on Stetson Avenue to the south. A 12-inch water line extends the length of Street A, connecting to all three points of connection around the site. A pump station is located within Planning Area 6 to boost the water pressure from the connection in Stetson Avenue up to the 1811 Pressure Zone. This assures potable, safe water access throughout all crucial areas within Emerald Acres.

2.4.2 SEWER PLAN DESCRIPTION

Sewer services will be provided to the Emerald Acres community by the EMWD. On-site facilities are

EMERALD ACRES

SPECIFIC PLAN NO.381

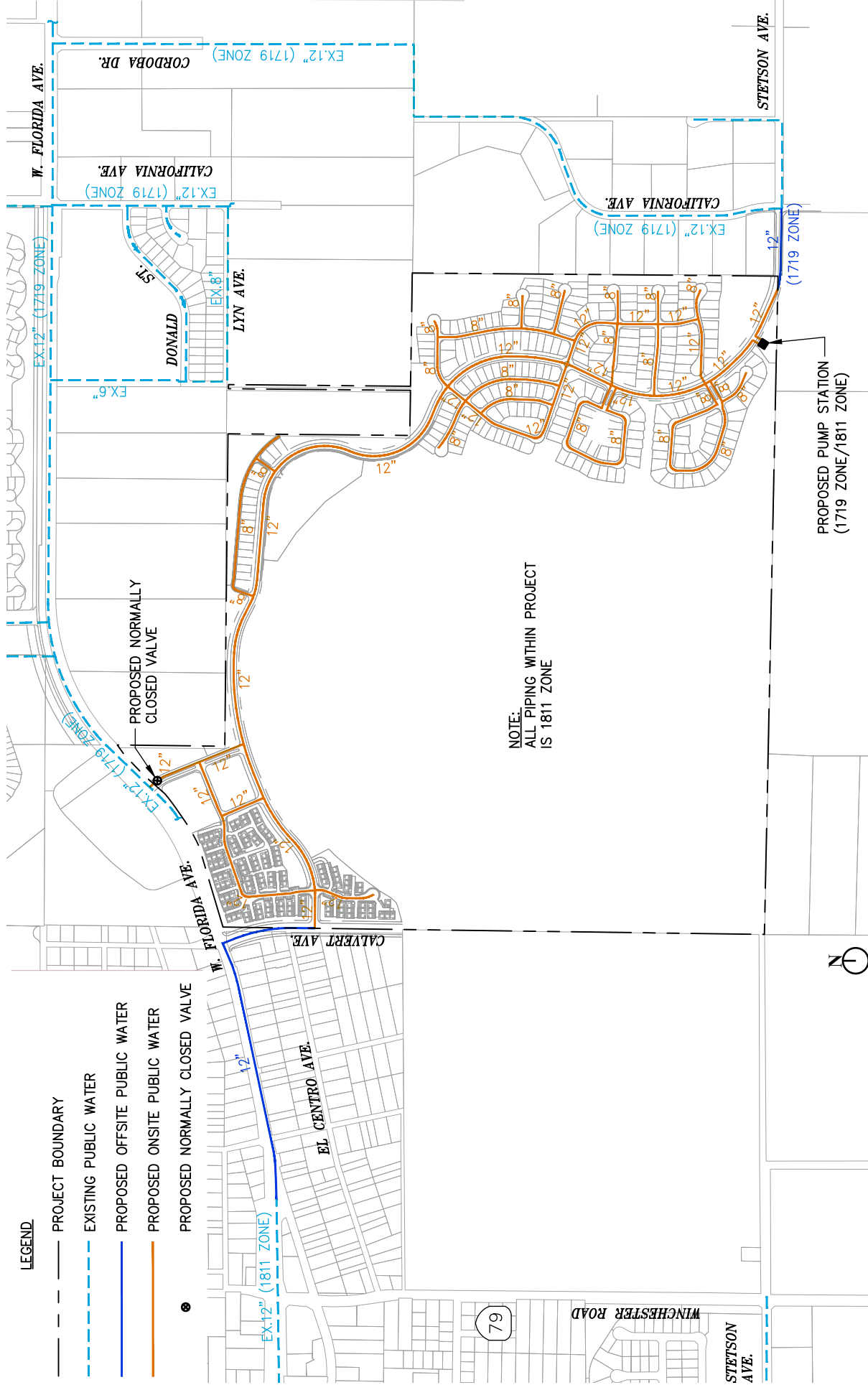
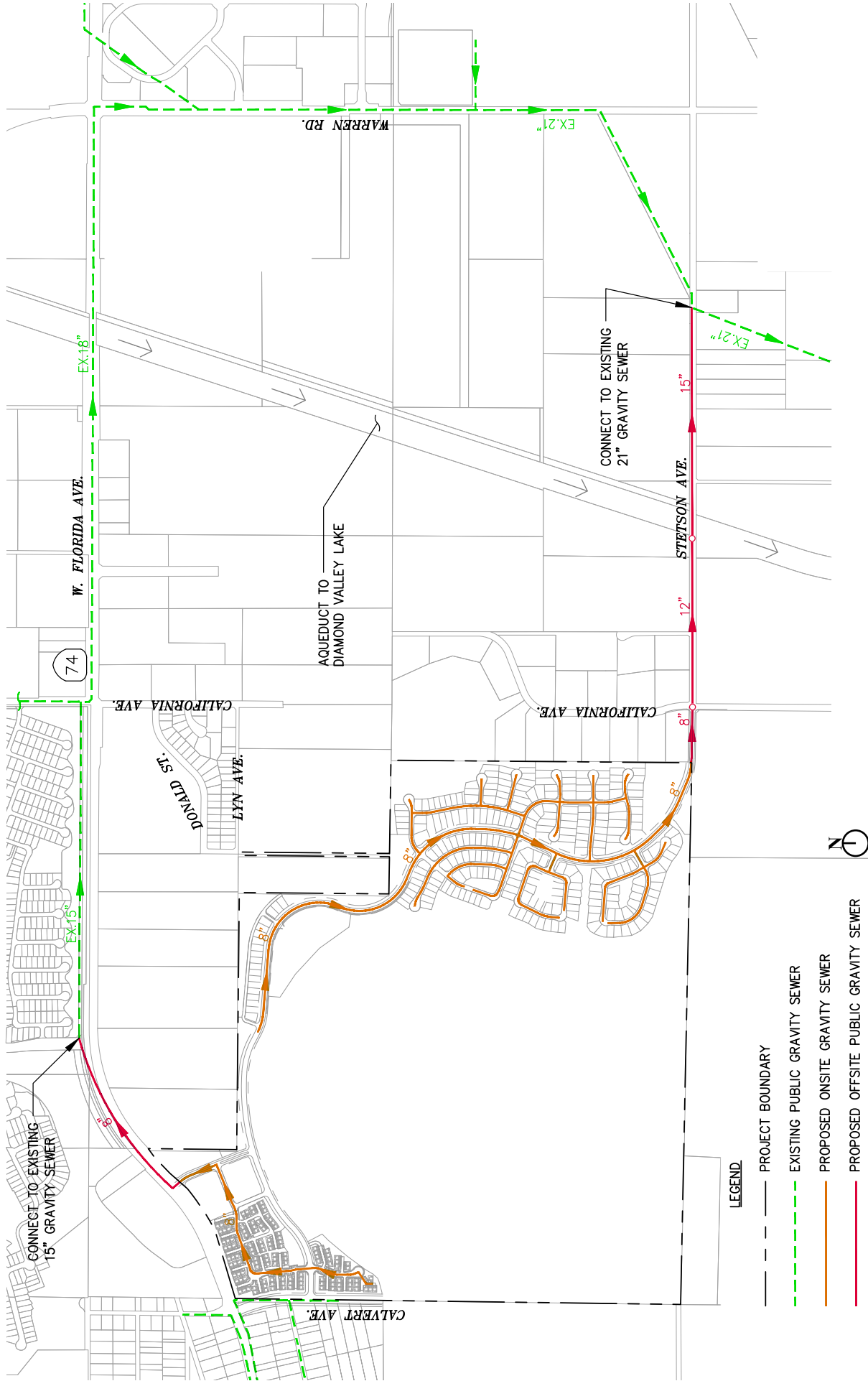


EXHIBIT 2-6. MASTER WATER PLAN

Source: Dexter Wilson Engineering, Inc.



LEGEND

- PROJECT BOUNDARY
- - - EXISTING PUBLIC GRAVITY SEWER
- PROPOSED ONSITE GRAVITY SEWER
- PROPOSED OFFSITE PUBLIC GRAVITY SEWER

EXHIBIT 2-7. MASTER SEWER PLAN

sized in accordance with EMWD criteria and based on future land uses. Off-site improvements include a gravity sewer main within a portion of Stetson Avenue that connects to an existing sewer line east of the Emerald Acres community.

2.4.2.1 POINTS OF CONNECTION

The Specific Plan is serviced by a network of existing sewer facilities, which includes an 8-inch lines within Florida Avenue. Exhibit 2-7, Master Sewer Plan, shows the location of on-site and off-site sewer facilities.

As shown on Exhibit 2-7, there are two main sewer lines that provide a connection from the Emerald Acres site to the EMWD network. The first sewer main begins in Village 4 and utilizes gravity lines to convey sewage northeasterly to the intersection of Florida Avenue and Street E, where an offsite 8-inch pipeline conveys sewage to a point of connection within Florida Avenue to the northeast. The second sewer main begins in Village 3 and utilizes gravity to convey sewage southeasterly (along Street A) to a point of connection approximately 0.40 miles east within Stetson Avenue, east of Warren Avenue. All sewer lines within the Emerald Acres community consist of 8-inch pipelines.

2.4.3 WATER AND SEWER DEVELOPMENT STANDARDS

1. Prior to installation of water and sewer lines, a geotechnical investigation shall be prepared for the proposed water and sewer line alignment to evaluate soil character, groundwater levels, and other factors.
2. All water and sewer lines shall be placed underground.
3. All water and sewer facilities shall be designed per the requirements of EMWD. Facility sizes and locations depicted on Exhibit 2-6, Master Water Plan, and Exhibit 2-7, Master Sewer Plan
4. Water and sewer line standards are conceptual and may be larger or smaller than indicated and may be relocated in accordance with EMWD requirements.
5. Water and sewer facilities shall be installed in accordance with the requirements and specifications of the Riverside County Building and Safety Department and the Riverside County Environmental Health Department.

2.5. OPEN SPACE AND RECREATION PLAN

2.5.1 OPEN SPACE AND RECREATION PLAN DESCRIPTION

An important element of the Emerald Acres community is the Open Space and Recreation Plan, Exhibit 2-8. Land uses and acreages are further defined in Table 2-2, Open Space and Recreation Plan Summary. The plan provides for a variety of recreational opportunities available for the enjoyment of the residents of the community which includes a 6.6-acre Neighborhood park area, 39.0 acres of Open Space - Conservation, 191.3 acres of open space for local habitat conservation, 3.2 acres of village recreation areas, and 2.2 acres of Multi-Use Trail.

The location of these uses can be seen in Exhibit 2-8. In all, 240.1 acres of the 335.5-acre community are designated for open space and recreational uses.

In accordance with the State Quimby Act and Riverside County Ordinance No. 460, residential development projects must provide 3.0 acres of park land per 1,000 residents. Furthermore, Riverside County policy and the Valley-Wide Recreation and Park District require that residential development projects provide 5.0 acres of park land per 1,000 residents. The Emerald Acres Specific Plan provides for the development of up to 391 homes. Using population generation rates contained within the Valley-Wide Recreation and Park

District Master Plan, Emerald Acres could generate up to 1,215 residents at build-out (268 homes x 3.2 persons per household + 123 townhomes x 2.9 persons per household = 1,215 persons), which corresponds to a park land requirement of 6.1 acres.

As shown on Exhibit 2-8, Emerald Acres contains a 6.6-acre public park, 3.2 acres of public Village Recreation Areas and a 2.2-acre trail network. Public recreation areas qualify for 100-percent credit (6.6 plus 3.2), while trail systems qualify for 50-percent credit (2.2 divided by 2) towards fulfilling park land dedication requirements.

Thus, the Emerald Acres Open Space and Recreation Plan complies with Ordinance No. 460 by providing a total of 10.9 acres of parkland qualifying for Quimby and County Park credit throughout the community (public parkland 9.8+ 1.1 acres for the trail system).

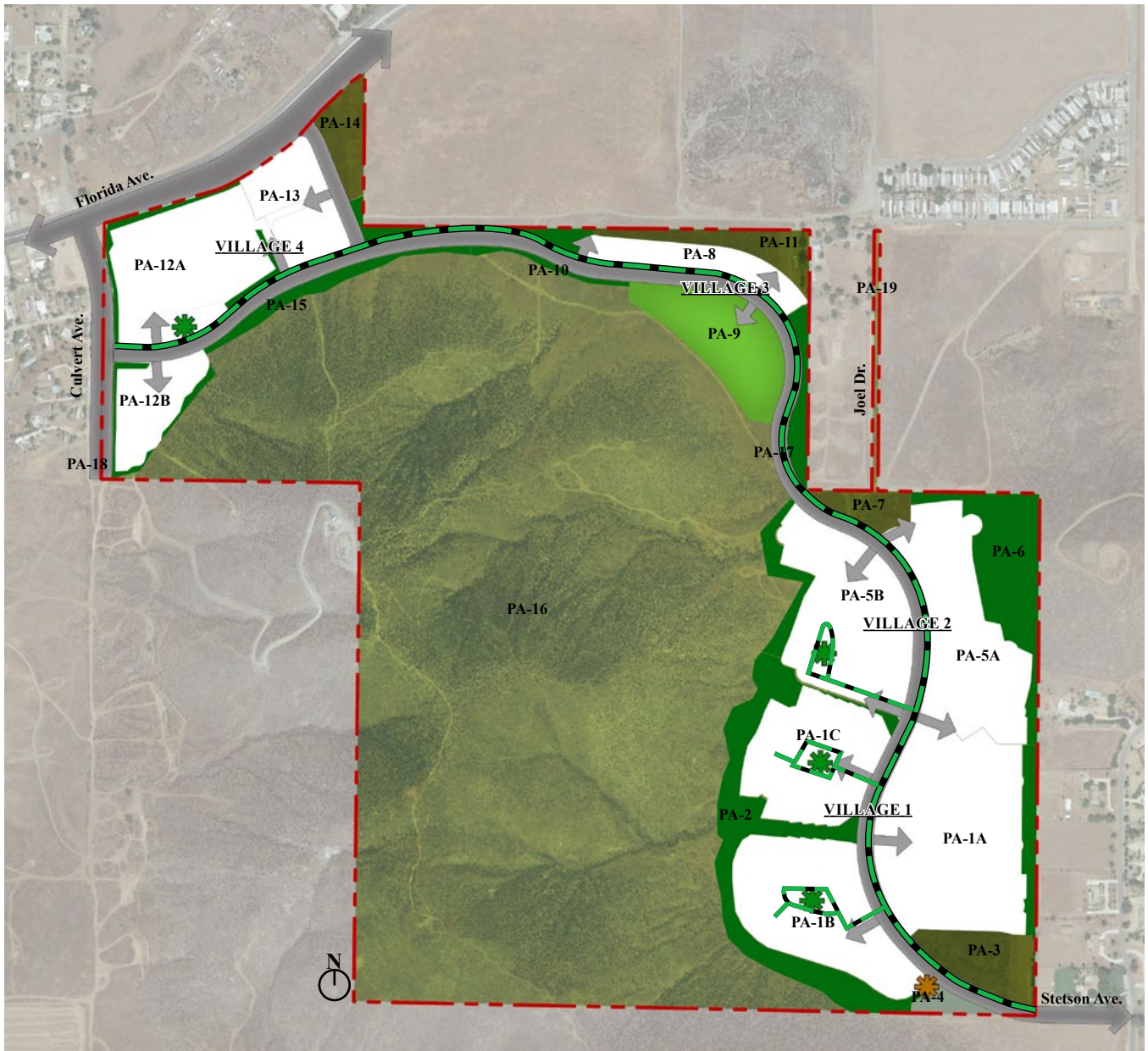
Prior to the official recordation of the Tentative Tract Map related to the Emerald Acres Specific Plan, an official Park Dedication Agreement with the Valley-Wide Recreation and Park District will occur.

TABLE 2-2. OPEN SPACE AND RECREATION PLAN SUMMARY

Planning Area/ Village	Land Use	Acres
Recreation		
PA 9 (Village 3)	Open Space - Recreation - Neighborhood Park	6.6
Village 1	Village Recreation Areas ¹	1.6
Village 2	Village Recreation Area ¹	0.7
Village 4	Village Recreation Area ¹	0.9
PA 18	Multi-Use Trail ²	2.2
Recreation Subtotal		12.0
Open Space		
PA 2	Open Space - Conservation (Slopes)	10.6
PA 3	Open Space - Conservation (WQMP Basin)	3.7
PA 6	Open Space - Conservation (Slopes)	7.8
PA 7	Open Space - Conservation (WQMP Basin)	1.7
PA 10	Open Space - Conservation (Slopes)	2.8
PA 11	Open Space - Conservation (WQMP Basin)	2.8
PA 14	Open Space - Conservation (WQMP Basin)	2.8
PA 15	Open Space - Conservation (Slopes)	6.8
PA 16	Open Space - Conservation Habitat	191.3
Open Space Subtotal		230.3
TOTAL	PROJECT TOTAL	242.3

¹ Village Recreation Areas are integrated within planning areas and land uses, and do not have their own land use designation.

² Multi-Use trail is part of Street A-Backbone Roads Planning Area and does not have its own land use designation.



LEGEND

- Open Space-Recreation (Neighborhood Park)
- Open Space-Conservation (Slopes)
- Open Space-Conservation (WQMP Basin)
- Open Space-Conservation Habitat (Natural Open Space)

- Village Recreation Area
- Equestrian Trail Access
- 10' Decomposed Granite Trail
- Project Boundary

EXHIBIT 2-8. OPEN SPACE AND RECREATION PLAN

2.5.1.1 OPEN SPACE – CONSERVATION

Open Space – Conservation areas comprise approximately 39.0 acres, within the Emerald Acres community. Open Space – Conservation areas serve the Emerald Acres community by providing natural aesthetics, stormwater percolation basins, manufactured slope areas, and Fire Fuel Modification Zones. Some Open Space – Conservation areas within Emerald Acres also include trails that allow formal pedestrian mobility and other recreational uses.

2.5.1.2 OPEN SPACE – CONSERVATION HABITAT

Open Space – Conservation Habitat areas comprise approximately 191.3 acres or 57% of the community. These open space areas consist primarily of rugged hillside terrain with slopes of 25% and greater, which will remain undisturbed and in their natural state. This hillside area is the southern extension of the Lakeview Hills, and has an elevation of approximately 2,080 feet above mean sea level. This open space serves as a dramatic backdrop for Emerald Acres, while defining the character of the community. The Open Space – Conservation Habitat areas within Emerald Acres will not include new trails or otherwise have any formal type of access for recreational use.

2.5.1.3 ACTIVE NEIGHBORHOOD PARK

A 6.6-acre Public Neighborhood Park, which will be owned, operated, and maintained by Valley Wide Recreation and Park District, is located in Planning Area 9, adjacent to Villages 1 and 2. Core amenities may include a basketball court, baseball field, soccer field, picnic grove, tot lot, restroom facilities, and on-site parking, as examples. Other design features are discretionary and may include community gathering areas, shade structures, passive recreation lawns, and barbeque area. Primary access to the Neighborhood Park is provided via the entrance located on Street A.

2.5.1.4 VILLAGE RECREATION AREAS

Village Recreation Areas are located within Village 1, Village 2 and Village 4 offer a variety of active and passive recreational opportunities for residents within Emerald Acres. These areas are provided within the Emerald Acres communities with 5,000 sq. ft. lots and attached or detached motorcourts. While open to the public, the Village Recreation Areas are intended to primarily serve the residents in the villages in which they are located. They may include but are not limited to elements such as picnic groves, dog parks, trail connections, shuffle board courts, open lawns, game table seating areas, community garden plots, and tot lots.

2.5.1.5 DETENTION BASINS

Stormwater management within Emerald Acres utilizes a system of detention basins, drainage channels and water quality basins, to control stormwater flows and eliminate historic flooding of adjacent properties. Some of the detention basins may contain landscaping designed to provide opportunities for passive recreation, so they can serve as passive recreation areas. Four detention basins are located within Planning Areas 3, 7, 11, and 14. In total, there are 10.6 acres of detention basins within the community.

2.5.1.6 TRAILS

A Multi-Use Trail network is provided throughout the Emerald Acres property that allows pedestrian access to all four (4) residential Villages, the commercial Planning Area 13, and the neighborhood recreational areas throughout the community. The Multi-Use Trail which traverses the entire Emerald Acres site, begins at the intersection of Street A and Stetson Avenue and continues along the northbound side of Street A before terminating at Calvert Avenue. Peripheral trail connections are located within Villages 1 and 2

and Planning Areas 9 and 13 to allow pedestrian access to the Village residential and recreational areas, the Emerald Acres public park, and other open-space areas. Multi-Use Trails shall be constructed from decomposed granite and be a minimum of 4-feet wide.

2.5.1.7 EQUESTRIAN TRAIL ACCESS

An Equestrian Trail is located along the southeast side of Village 1 (between PA 1B and PA 4) and connects to the native trail network south of the Project site. The trail point-of-entry is located between PA 1B and PA 4, along Street A, and the native trail continues south of Village 1 before meandering to the natural open space within Planning Area 2. The native equestrian trail access point is open to the public and will be maintained by a landscape maintenance district or functional equivalent.

2.5.2 OPEN SPACE AND RECREATION PLANS DEVELOPMENT STANDARDS

1. Final park land requirements for Emerald Acres shall be determined by the County of Riverside at the time implementing tentative tract maps are submitted to the County of Riverside for review, based on the final number of dwelling units and corresponding population generated by Emerald Acres. In the event that the County of Riverside determines that the community is deficient in required park land, the implementing project shall meet the Quimby Act requirements through any combination of the following options: dedicate additional qualifying park land within the project area; pay Quimby Act in-lieu fees to offset the deficiency; and/or improve the dedicated park land as a credit against the required Quimby Act in-lieu fees.
2. The Neighborhood Park area within Specific Plan (PA 9) shall be mass graded by the Master Developer and dedicated to the Valley-Wide Recreation and Park District in order to satisfy the requirements within the Quimby Act. Other recreational facilities within each Village will be built by the developer / merchant builder of that Village.
3. The Neighborhood Public Park shall be maintained for the benefit of the public at large and Village Recreation Areas shall be maintained for the benefit of all residents within the Emerald Acres community. Ownership and maintenance of all recreational facilities will be the responsibility of a Master Homeowners' Association and the Valley-Wide Park District. The maintenance mechanism will be selected at the time that the implementing development application is submitted.
4. Village Recreation Areas within each Village will be completed prior to the issuance of the first Certificate of Occupancy within its respective Village.
5. All recreational facilities will be landscaped and, where necessary, irrigated in a manner that is conducive to the type of plant material and landscape setting.
6. Landscaping within recreation and open space areas will be further governed by Section 4, Design Guidelines, of this Specific Plan.
7. This Specific Plan shall conform to the standards set forth by Riverside County Ordinance No. 859.
8. Conceptual landscaping plans for recreation facilities/areas shall be submitted in conjunction with tentative map applications that implement any portion of this Specific Plan.
9. Where appropriate, conservation easements will be required, during the Tentative Map process, to preserve natural drainages and habitat.
10. No new trails or other formal access are permitted within Open Space-Conservation Habitat areas within the Specific Plan area.

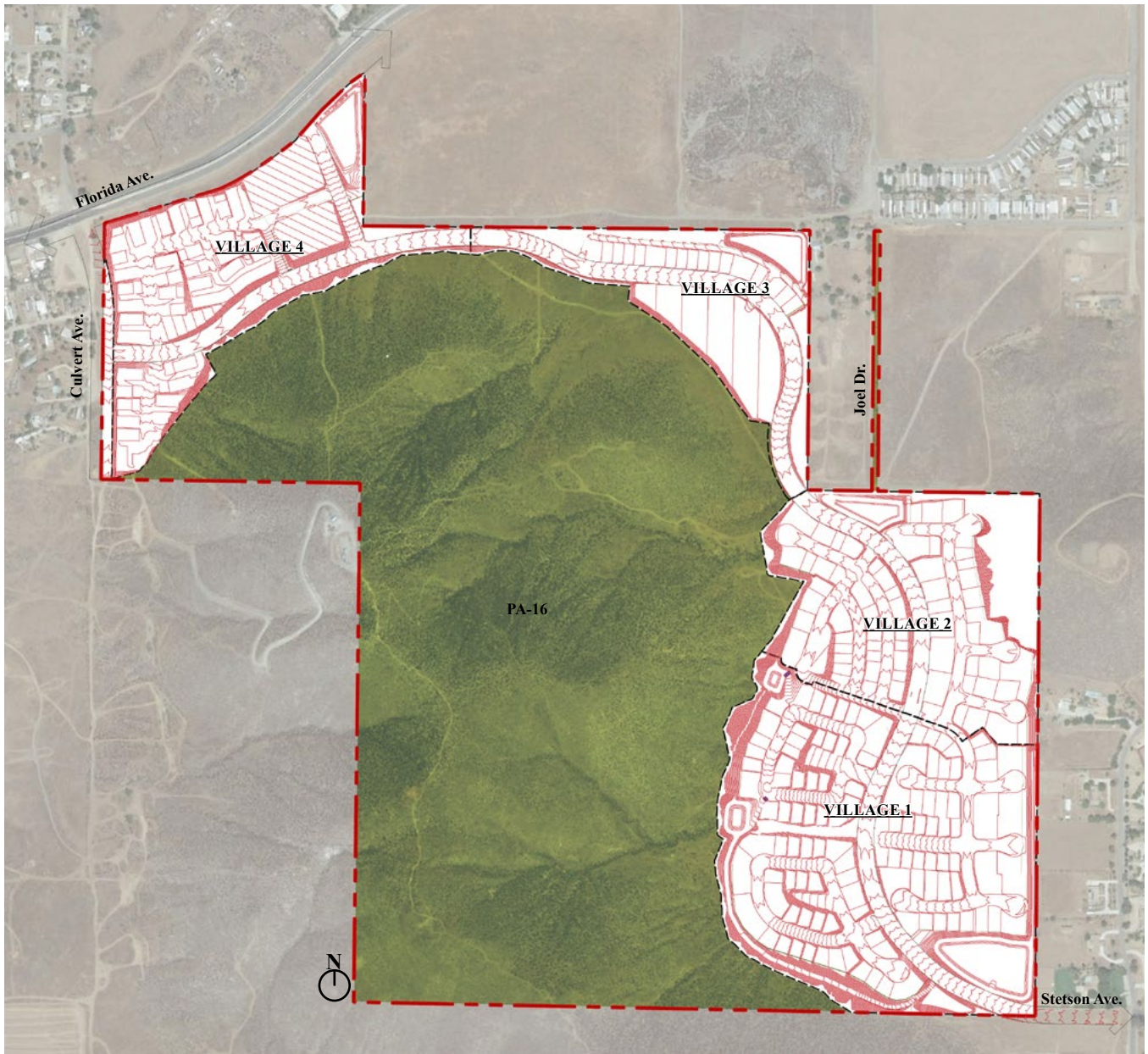
2.6. GRADING PLAN

2.6.1 GRADING PLAN DESCRIPTION

The Emerald Acres Specific Plan grading plan is tailored to the existing topography of the community. It is intended that proposed site grading be sensitive to natural landforms as depicted in Exhibit 2-9, Conceptual Grading Plan. The majority of the community's open space acreage, which includes most areas with slopes of 25% or greater, has been left natural and undeveloped to preserve the backdrop of the Lakeview Mountains, scenic topographic features, rock outcroppings, and biological resources found in those areas. Although erosion control measures will be applied to portions of undeveloped slope areas adjacent to major roadways and residential planning areas within the community, these disturbed areas will be re-vegetated and owned and maintained by the Master Homeowners' Association.

2.6.2 GRADING PLAN DEVELOPMENT STANDARDS

1. All grading activities shall conform to Riverside County standards, shall be in substantial conformance with the overall Conceptual Grading Plan (see Exhibit 2-9) and shall implement any grading-related mitigation measures identified in EIR No. XXX and associated geotechnical studies.
2. Prior to initial grading activities, a detailed soils report and geotechnical study shall be prepared for each implementing tentative map to analyze on-site soil conditions and slope stability. The soils report and geotechnical study required for implementing development projects shall include appropriate measures to control erosion and dust.
3. Prior to any development within any planning area of this Specific Plan, an overall Conceptual Grading Plan for that planning area shall be submitted for Planning Department approval. The Grading Plan for each planning area shall be used as a guideline for subsequent detailed grading plans for individual stages of development within that planning area, and shall include: techniques employed to prevent erosion and sedimentation as well as eliminate source pollutants during and after the grading process; approximate time frames for grading; identification of areas which shall be graded during high probability rain months (January through March); and preliminary pad and roadway elevations. Grading work shall be balanced on-site whenever possible.
4. All public roads within the Specific Plan area shall have a gradient not to exceed 15%.
5. Slopes steeper than 2:1 or exceeding fifteen feet (15') in vertical height are allowed provided they are recommended to be safe in a slope stability report prepared by a soils engineer or an engineering geologist. The slope stability report shall also contain recommendations for landscaping and erosion control. Riverside County Ordinance No. 457 will be observed regarding setback and landscaping requirements with respect to slopes.
6. Where cut and fill slopes are created higher than three feet, detailed landscaping and irrigation plans shall be submitted to the Planning Department prior to Grading Plan approval. The plans shall be reviewed for type and density of ground cover, shrubs, and trees.
7. The Developer shall be responsible for maintenance and upkeep of all planting and irrigation systems until those operations are the responsibilities of other parties.
8. Where cut and fill slopes are created higher than three feet, detailed landscaping and irrigation plans shall be submitted to the Planning Department prior to Grading Plan approval. Plans shall be reviewed for type and density of ground cover, shrubs, and trees.
9. Potential brow ditches, terrace drains, or other minor swales, determined necessary by the County of Riverside shall be lined with natural erosion control materials or concrete.



LEGEND




-  Proposed Grading Contours
-  Village Boundaries
-  Project Boundary

EXHIBIT 2-9. CONCEPTUAL GRADING PLAN

10. Graded land that is undeveloped shall be maintained and planted with interim landscaping within ninety (90) days of completion of grading, unless building permits are obtained.
11. A grading permit shall be obtained from the County of Riverside, as required by the County Grading Ordinance, prior to grading.
12. Pursuant to requirements of the State Water Resources Control Board, a State-wide general National Pollution Discharge Elimination System (NPDES) construction permit will apply to construction activities associated with the proposed project. Construction activities include cleaning, grading, or excavation that results in the disturbance of at least one acre of total land area or activity which is part of a larger common plan of development of one acre or greater. Therefore, the developer or builder of the project shall be required to obtain the appropriate State NPDES permits prior to commencing grading activities.

2.7. MAINTENANCE PLAN

Successful operation of maintenance districts and associations are important to maintain quality in a Specific Plan area. It is anticipated that maintenance responsibilities for public roadways will be maintained by the County of Riverside. Other common Project facilities shall be divided among a Master Homeowners' Association, Neighborhood Associations, and/or similar maintenance mechanisms. Final decisions regarding maintenance entities shall be made at a future stage of Specific Plan design review and in concert with appropriate agencies. See Table 2-3, Maintenance Plan Summary, for a summary of maintenance responsibilities.

TABLE 2-3. MAINTENANCE PLAN SUMMARY

	Homeowners' Association	Public or Quasi-public Agency	Riverside County	Other Service Entity
Common Open Space	✓	✓		
Open Space - Conservation		✓		✓
Landscaped Open Spaces	✓	✓		
Village Recreation Areas	✓	✓		✓
Detention Basins		✓	✓	
Neighborhood Park		✓		
Specific Plan Signage	✓			
Sewer/Water			✓	
Sidewalks and Hardscape	✓		✓	
Storm Drains		✓		
Street Lighting			✓	
Streets (Private)	✓			
Streets (Public)			✓	

2.7.1 MASTER HOMEOWNERS' ASSOCIATION

A Master Homeowner's Association is required to maintain private roadways and alleys as well as parkway areas outside of the right-of-way and manufactured slope areas. Common areas identified in the Specific Plan shall be owned and maintained by a permanent public or private master maintenance organization, to assume ownership and maintenance responsibility for all common recreation, open space, private circulation systems, and landscape areas. Areas of responsibility shall include open space, Specific Plan signage, private recreation facilities, and landscape areas located along the Specific Plan roadways.

2.7.2 RESIDENTIAL NEIGHBORHOODS ASSOCIATION

In Villages 1 through 3 a smaller association may be formed; however, in Village 4, a smaller association shall be formed to assume maintenance responsibility for common areas and facilities that benefit only residents of this Village. Common open space areas, recreational facilities, and private roadways exemplify facilities that would come under the jurisdiction of a neighborhood association.

2.7.3 PARKS AND OPEN SPACE

Village Recreation Areas and open space areas will be the responsibility of a Homeowners' Association or a quasi-public agency. The public Neighborhood Park will be the responsibility of the Valley-Wide Recreation and Park District.

2.7.4 PUBLIC AND PRIVATE ROADWAYS

All public and private roadways will be designed and constructed to the Engineering Standards as described in Section 3 of this Specific Plan. All public roadways shall be maintained by the County of Riverside whereas all private roadways and accompanying sidewalks and landscaping shall be maintained by the Homeowners' Association.

SECTION 3: DEVELOPMENT STANDARDS

The Emerald Acres Specific Plan consists of four (4) residential Villages including one (1) Neighborhood Park in Village 3 and one (1) commercial Planning Area in Village 4 and various open space land use areas. The four Village locations were formed on the basis of logical, separate areas of development. Adjacent to each Village are Planning Areas which are associated with other land uses such as open space, recreational, and commercial uses.

The boundaries for each Village and/or Planning Area, as shown on Exhibit 3-1, Exhibit 3-3, Exhibit 3-6, Exhibit 3-8, Exhibit 3-11, and Exhibit 3-13, reflect the projects full development envelope, including manufactured slope areas. Criteria considered in this process included the following: uniformity of use as it pertains to zoning, relationship to adjoining product, and relationship to surrounding topography.

This section addresses Development Standards for each Planning Area, including but not limited to, minimum lot sizes, and primary and secondary vehicular access points. The development standards for each Planning Area also provides references to relevant exhibits, including exhibits that depict desired architecture and landscaping elements, contained throughout this Specific Plan.

A Specific Plan Zoning Ordinance has also been prepared along with this Specific Plan document. The zoning provisions within that ordinance establish use restrictions for each Village and Planning Area. The zoning provisions should be used in conjunction with the planning standards for each respective Village and/or Planning Area. A summary of the land uses, densities, and lot sizes is shown on Table 2-1, Detailed Land Use Summary.

3.1. VILLAGE 1 – 6,000 SF SINGLE FAMILY DETACHED

3.1.1 DESCRIPTIVE SUMMARY

Village 1, as shown on Exhibit 3-1, Village 1 Summary, consists of three (3) residential neighborhoods (1A, 1B and 1C) located within the southeastern portion of Emerald Acres, along Street A. Village 1 is bisected by Street A and is accessible via three main entrances along Street A and the southernmost roundabout along Street A. The eastern and western edges of Village 1 are manufactured slopes encompassed within Planning Area 2. The Water Quality Management Plan (WQMP) basin, Planning Area 3, is located at the southeastern corner of Village 1. The northern boundary of Village 1 is directly adjacent to Village 2, and the western boundary is adjacent to Planning Area 16, which is the Natural Open Space parcel on the site.. Villages 1B and 1C contain Recreation Areas which may contain amenities like tot lots, seating, lawn, etc.

As shown on Exhibit 3-2, Village 1 Development Standards, Village 1 provides for the development of single-family detached homes in a traditional neighborhood configuration, on minimum lot sizes of 6,000 square feet. Overall, Village 1 provides for a maximum of 144 dwelling units within 35.8 acres of the Emerald Acres property, with a density of 4.0 du/ac.

Due to the wildland fire risk associated with residential land uses adjacent to natural open space, the implementation of Fire Fuel Modification Zones is required at the interface between residential uses within Village 1 and natural open space within Planning Area 16. Fire Fuel Modification Zone requirements are depicted on Exhibit 4-17, Residential/Open Space Edge Condition with 100' Fire Fuel Modification Zone, and generally require a reduction of vegetation within a 100' zone within the manufactured slope areas adjacent to residential uses that serves to reduce wild land fire risk to residential structures.

3.1.2 LAND USE DEVELOPMENT STANDARDS

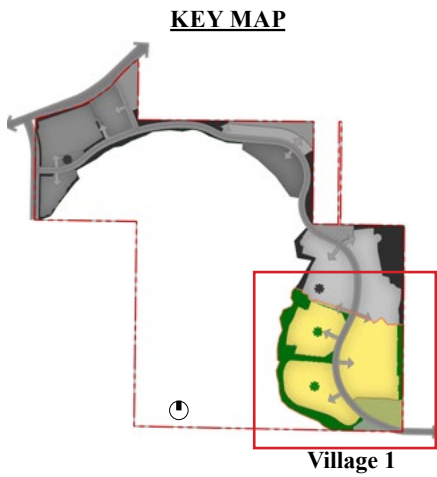
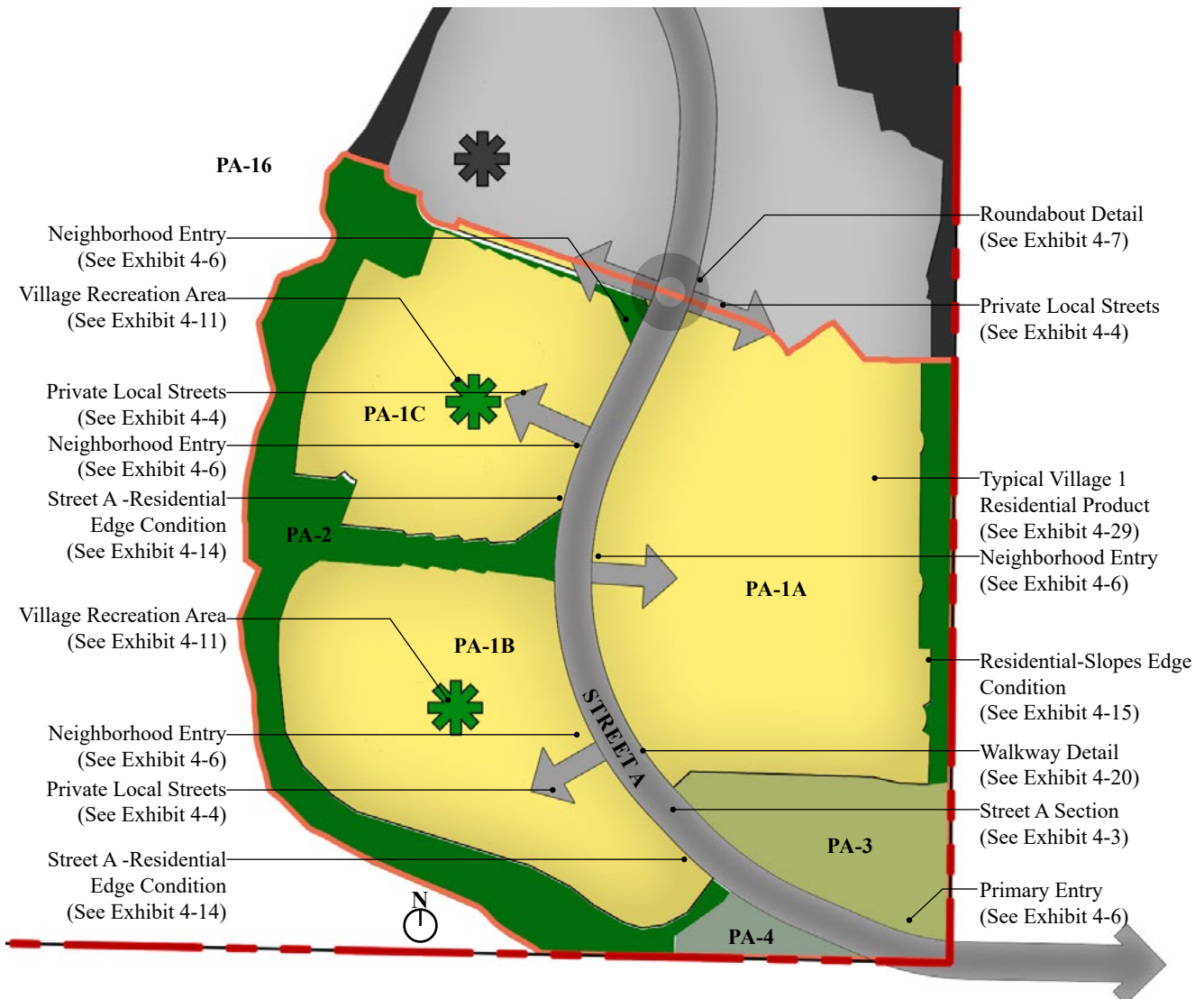
Please refer to Ordinance No. 348.4947/50, Article XVIIa SP Zone (Specific Plan).

3.1.3 PLANNING STANDARDS

1. Site plan concepts for residential development within Village 1 are depicted on Exhibit 3-2, Village 1 Development Standards.
2. Access to Village 1 is provided by Street A and private streets.
3. Walls and fencing located within Emerald Acres shall be constructed as delineated on Exhibit 4-18, Conceptual Wall and Fence Plan. Access to open space will be restricted by using view fencing.
4. A minimum of two 5,000 sq. ft. minimum Village Recreation Areas shall be provided within Village 1.
5. A Primary entry may be provided at the Village entrance as illustrated on Exhibit 4-1, Conceptual Landscape Key Map. See Exhibit 4-6, Primary and Neighborhood Entry, for a conceptual design of entry monumentation for the Project area.
6. Primary and Neighborhood entries may be provided at the entrances along Street A as illustrated on Exhibit 4-6, Primary and Neighborhood Entry.
7. Roundabout treatments shall be provided along Street A as illustrated on Exhibit 4-7, Roundabout Detail.
8. Exhibit 4-1, Conceptual Landscape Key Map, depicts the location of edge treatments between Village 1 and open space areas. See Exhibit 4-15, Residential/Manufactured Slope/Detention Basin Edge Condition, for detailed standards on these edge conditions.
9. Edge treatments between Village 1 and Street A shall be provided. See Exhibit 4-14, Street A/Residential Edge Condition, for detailed standards on these edge conditions.
10. To ensure adequate defensible space between residential structures and naturally vegetated open space, Fire Fuel Modification Zones (FMZ) shall be implemented that incorporate both manufactured and natural slopes adjacent to the natural open spaces in Planning Areas 2, 6, 10, 15 and 16 as depicted in Exhibit 4-1, Conceptual Landscape Key Map.
11. Fire Fuel Modification Zone edge conditions shall be established as illustrated on Exhibit 4-17, Residential/Open Space Edge Condition with 100' Fire Fuel Modification Zone.
12. Public roadway landscape treatments, as shown in Exhibit 4-3, Stetson Avenue, Street 'A' and Street 'E' Street Sections shall be provided along Street A.
13. Private roadway landscape treatments, as shown in Exhibit 4-4, Private Local Streets and Alley Street Sections are planned along Local Streets and Private Drives.
14. Where manufactured slopes occur within Village 1, criteria will follow slope guidelines within Section 4.2.9, Manufactured Slopes.
15. Primary access to open spaces shall be provided by adjacent village sidewalks.
16. Trails and sidewalks shall be provided as illustrated in Exhibit 4-20, Walkway Detail.
17. Please refer to Section 4, Design Guidelines for other related design criteria.

18. Please refer to Section 2, Specific Plan for the following standards that apply site-wide:

- 2-1. Land Use Plan
- 2-2. Circulation Plan
- 2-3. Drainage Plan
- 2-4. Water and Sewer Plan
- 2-5. Open Space and Recreation Plan
- 2-6. Grading Plan
- 2-7. Maintenance Plan



LAND USE SUMMARY

PA-1A, 1B & 1C	35.8 Acres
Medium Density Residential (6,000 SF Lots)	144 Units
	4.0 DU/AC
PA-2	
Open Space-Conservation (Slopes)	10.6 Acres
PA-3	
Open Space-Conservation (WQMP Basin)	3.7 Acres
PA-4	
Public Facilities	0.8 Acres
PA-17 (Portion) Backbone Roads	3.9 Acres
Total	54.8 Acres

LEGEND

	Project Boundary
	Village 1 Boundary
	Medium Density Residential (6,000 SF Lots)
	Village Recreation Area
	Open Space-Conservation (WQMP Basin)
	Open Space-Conservation (Slopes)
	Public Facilities

EXHIBIT 3-1. VILLAGE 1 SUMMARY



CONCEPTUAL PRODUCT LAYOUT

RESIDENTIAL DEVELOPMENT STANDARDS-VILLAGE 1

Product Type - Single Family Detached	
Maximum Village Density	5.0 du/ac
Maximum Lot Area	6,000 sq.ft.
Minimum Lot Dimensions	60'X100'
Setbacks	
Minimum Front Yard (Back of Sidewalk to Building)	15'
Minimum Corner Side Yard (Back of Sidewalk to Building)	10'
Minimum Interior Side Yard	5'
Minimum Rear Yard	10'
Minimum Front Garage (Back of Sidewalk to Building)	20'
Minimum Side Garage (Back of Sidewalk to Building)	15'
Other	
Maximum Structural Height	35'
Maximum Lot Coverage	65%
Encroachments (Fireplaces, AC Units, Media Centers)	2'
Encroachments (Balconies, Porches, Decks)	5'

KEY MAP

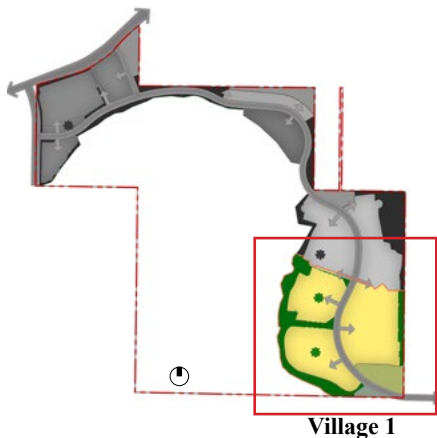


EXHIBIT 3-2. VILLAGE 1 DEVELOPMENT STANDARDS

3.2. VILLAGE 2 – 5,000 SF SINGLE-FAMILY DETACHED

3.2.1 DESCRIPTIVE SUMMARY

Village 2, as shown on Exhibit 3-3, Village 2 Summary, consists of two (2) residential neighborhoods (5A and 5B), located within the eastern portion of Emerald Acres, between Villages 1 and 3. Village 2 is bisected by Street A and is accessible via two primary entrances from Street A. The WQMP Basin PA 7 is at the northwestern boundary of Village 2, the western boundary is adjacent to PA 16, which is the Natural Open Space parcel on the site, and the Open Space-Slopes are in parcel PA 6 along the east and west side of Village 2. PA 5B in Village 2 neighborhood contains a Recreation Area which may offer amenities such as tot lots, seating, community garden, etc.

As shown on Exhibit 3-4, Village 2 Development Standards, Village 2 provides for the development of single family detached homes, arranged in a traditional front loaded configuration, on minimum lot sizes of 5,000 square feet. Overall, Village 2 will consist of a maximum of 105 dwelling units within 22.2 acres of the Emerald Acres property, allowing a total density of 4.7 du/ac. Alley loaded product, not exceeding the maximum allowable units in Village 2, may be plotted upon approval of an improvement plan for such product proposed by the builder. See Exhibit 3-5 for alley loaded product development standards.

Due to the wildland fire risk associated with residential land uses adjacent to natural open space, the implementation of Fire Fuel Modification Zones is required at the interface between residential uses within Village 2 and natural open space within Planning Area 6 and 16. Fire Fuel Modification Zone requirements are depicted on Exhibit 4-17, Residential/Open Space Edge Condition with 100' Fire Fuel Modification Zone, and generally require a reduction of vegetation within a 100' zone within the manufactured slope areas adjacent to residential uses that serves to reduce wild land fire risk to residential structures.

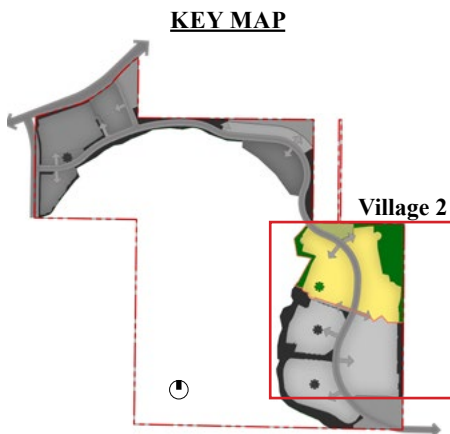
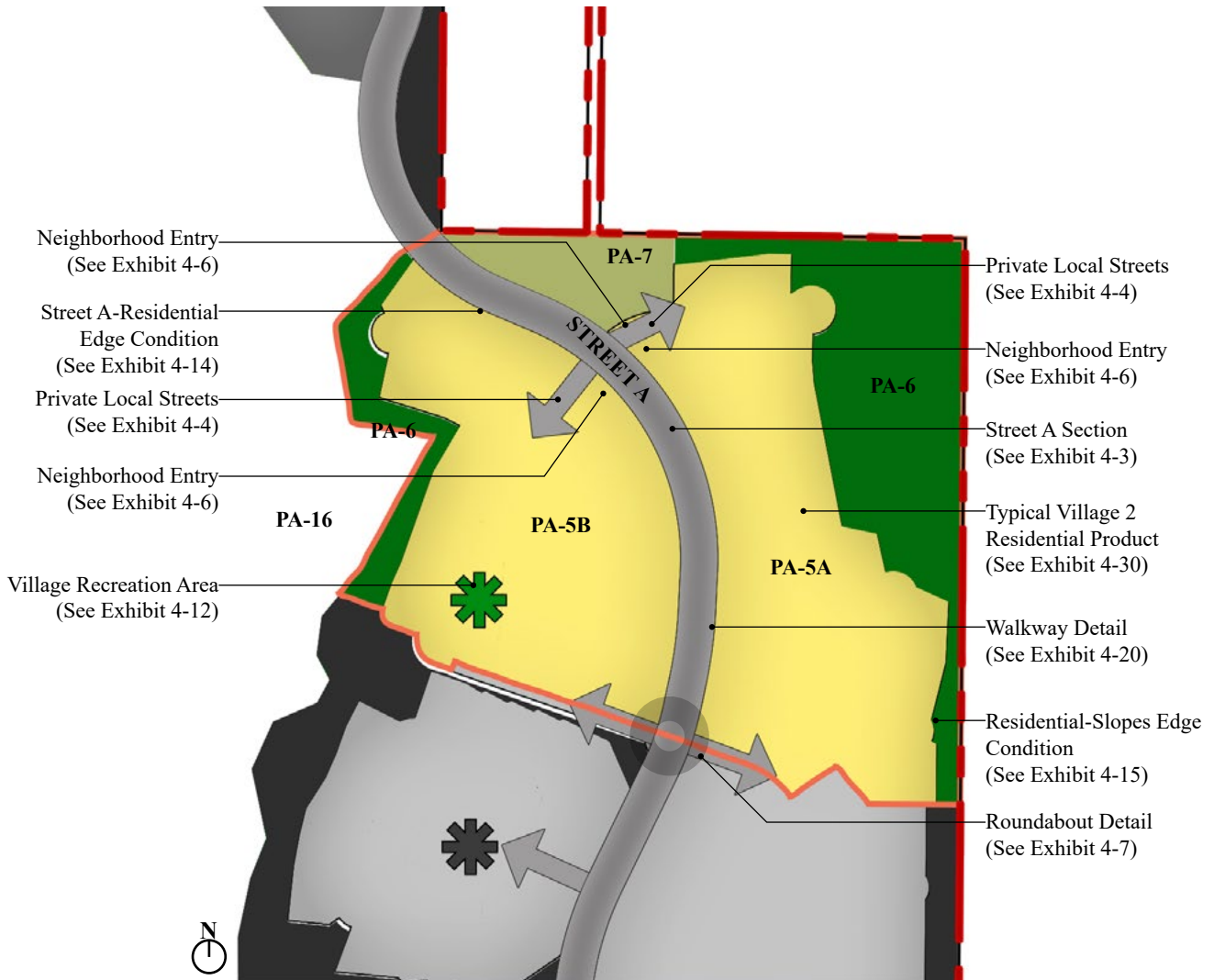
3.2.2 LAND USE AND DEVELOPMENT STANDARDS

Please refer to Ordinance No. 348.4947/50, Article XVIIa SP Zone (Specific Plan).

3.2.3 PLANNING STANDARDS

1. Site plan concepts for residential development within Village 2 are depicted on Exhibit 3-4, Village 2 Development Standards.
2. Primary access to Village 2 shall be provided via Street A and private roads with pedestrian access along the front yards via meandering paseos and/or sidewalks.
3. Walls and fencing located within Emerald Acres shall be constructed as delineated on Exhibit 4-18, Conceptual Wall and Fence Plan. Access to open space will be restricted by using view fencing.
4. Exhibit 4-1, Conceptual Landscape Key Map, depicts the location of edge treatments between Village 2 and adjacent open space areas. See Exhibit 4-15, Residential/Manufactured Slope/Detention Basin Edge Condition, for detailed standards on these edge conditions.
5. Edge treatments between Village 2 and Street A shall be provided. See Exhibit 4-14, Street A/Residential Edge Condition, for detailed standards on these edge conditions.
6. Neighborhood entry monuments may be provided at the entrances along Street A as illustrated on Exhibit 4-6, Primary and Neighborhood Entry.
7. At least two 5,000 sq. ft. minimum Village Recreation Areas shall be provided within Village 2.
8. Trails and sidewalks shall be provided as illustrated in Exhibit 4-20, Walkway Detail.

9. Public roadway landscape treatments, as shown in Exhibit 4-3, Stetson Avenue, Streets 'A' and 'E' Street Sections, shall be provided along Street A.
10. Private roadway landscape treatments, as shown in Exhibit 4-4, Private Local Streets and Alley Street Sections, are planned along Local Streets and Private Drives.
11. Fire Fuel Modification Zone edge conditions shall be established as illustrated on Exhibit 4-17, Residential/Open Space Edge Condition with 100' Fire Fuel Modification Zone.
12. Where manufactured slopes occur, criteria will follow slope guidelines within Section 4.2.9, Manufactured Slopes.
13. Please refer to Section 4, Design Guidelines, for other related design criteria.
14. Please refer to Section 2, Specific Plan, for the following standards that apply site-wide:
 - 2-1. Land Use Plan
 - 2-2. Circulation Plan
 - 2-3. Drainage Plan
 - 2-4. Water and Sewer Plan
 - 2-5. Open Space and Recreation Plan
 - 2-6. Grading Plan
 - 2-7. Maintenance Plan



LAND USE SUMMARY

PA-4A & 4B	22.2 Acres
Medium Density Residential (5,000 SF Lots)	105 Units
	4.7 DU/AC
PA-5	7.8 Acres
Open Space-Conservation (Slopes)	
PA-6	1.7 Acres
Open Space-Conservation (WQMP Basin)	
PA-17 (Portion) Backbone Roads	2.8 Acres
Total	34.5 Acres

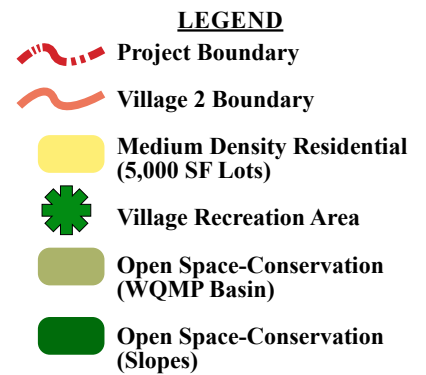


EXHIBIT 3-3. VILLAGE 2 SUMMARY



CONCEPTUAL PRODUCT LAYOUT

RESIDENTIAL DEVELOPMENT STANDARDS-VILLAGE 2

Product Type - Single Family Detached	
Maximum Village Density	5.0 du/ac
Maximum Lot Area	5,000 sq.ft.
Minimum Lot Dimensions	50'X100'
Setbacks	
Minimum Front Yard (Back of Sidewalk to Building)	10'
Minimum Corner Side Yard (Back of Sidewalk to Building)	10'
Minimum Interior Side Yard	5'
Minimum Rear Yard	10'
Minimum Front Garage (Back of Sidewalk to Building)	20'
Minimum Side Garage (Back of Sidewalk to Building)	10'
Other	
Maximum Structural Height	35'
Maximum Lot Coverage	65%
Encroachments (Fireplaces, AC Units, Media Centers)	2'
Encroachments (Balconies, Porches, Decks)	5'

KEY MAP

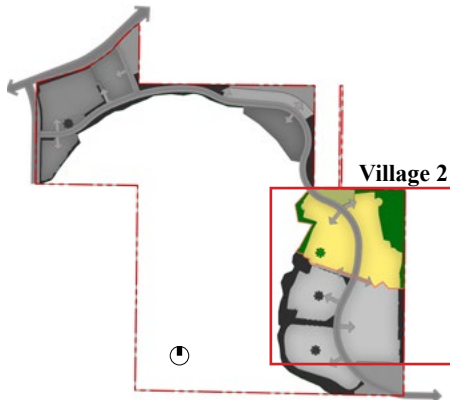
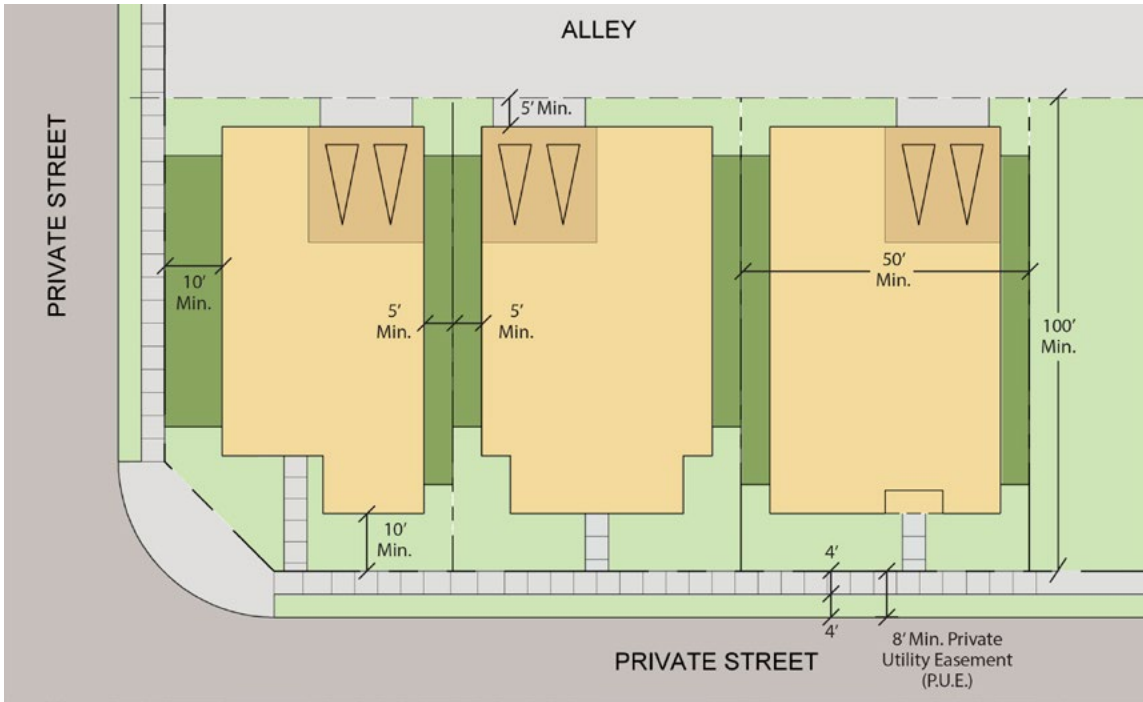


EXHIBIT 3-4. VILLAGE 2 DEVELOPMENT STANDARDS - CONVENTIONAL



CONCEPTUAL PRODUCT LAYOUT

RESIDENTIAL DEVELOPMENT STANDARDS-VILLAGE 2

Product Type - Single Family Detached-Alley Loaded	
Maximum Village Density	5.0 du/ac
Maximum Lot Area	5,000 sq.ft.
Minimum Lot Dimensions	50'X100'
Setbacks	
Minimum Front Yard (Back of Sidewalk to Building)	10'
Minimum Corner Side Yard (Back of Sidewalk to Building)	10'
Minimum Interior Side Yard	5'
Minimum Rear Garage (Back of Sidewalk to Building)	5'
Minimum Side Garage (Back of Sidewalk to Building)	10'
Other	
Maximum Structural Height	35'
Maximum Lot Coverage	65%
Encroachments (Fireplaces, AC Units, Media Centers)	2'
Encroachments (Balconies, Porches, Decks)	5'

KEY MAP

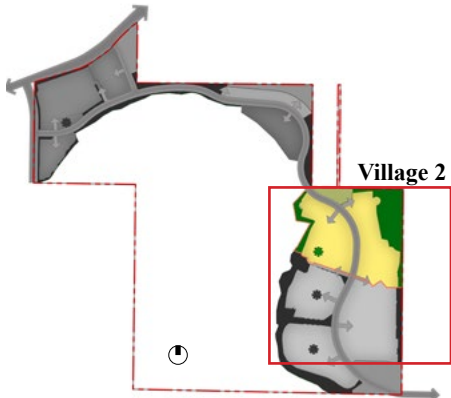


EXHIBIT 3-5. VILLAGE 2 DEVELOPMENT STANDARDS - ALLEY LOADED

3.3. VILLAGE 3 – 5,000 SF SINGLE FAMILY DETACHED

3.3.1 DESCRIPTIVE SUMMARY

Village 3, as shown on Exhibit 3-6, Village 3 Summary, is a residential Planning Area located within the northeastern portion of Emerald Acres, north of Village 2. Village 3 residential parcel PA 8 is located along the north side of Street A and is accessible via two entries from Street A. PA 9 along the Southwestern boundary is a 6.6-acre Neighborhood Park adjacent to PA 16, which is the Natural Open Space parcel on the site. A WQMP Basin is located in PA 11 along the northern Village 3 boundary. PA 10 encompasses the Open Space – Slopes in Village 3.

As shown on Exhibit 3-7, Village 3 Development Standards, Village 3 provides for the development of single-family detached homes arranged as a traditional single-family neighborhood, on minimum lot sizes of 5,000 square feet.

Overall, Village 3 will consist of a maximum of 19 dwelling units within 3.7 acres of the Emerald Acres property, allowing a total density of 5.0du/ac.

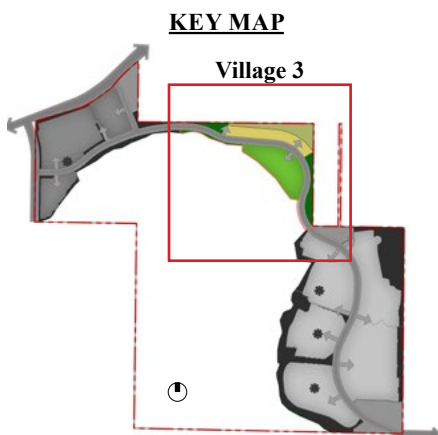
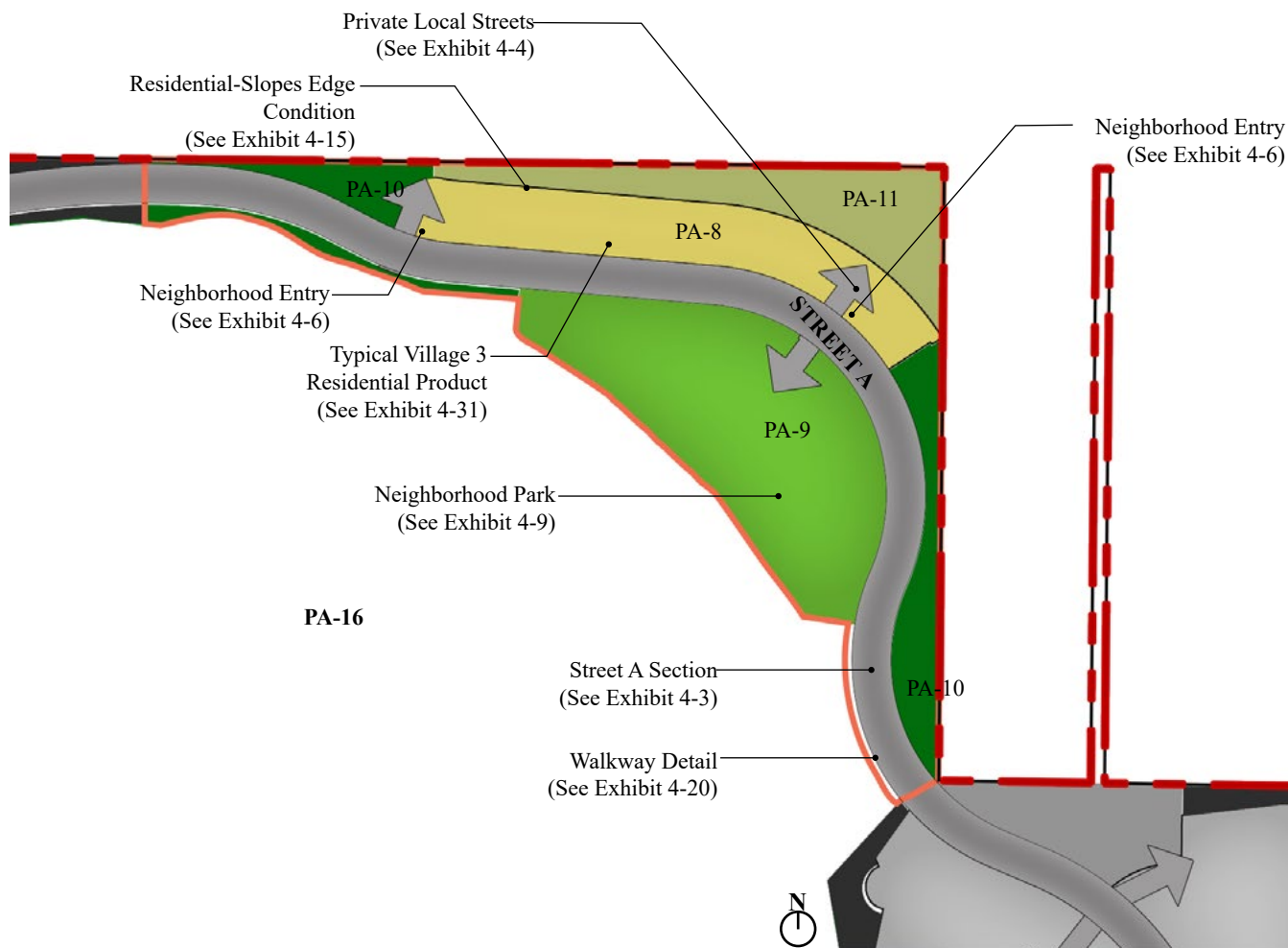
3.3.2 LAND USE AND DEVELOPMENT STANDARDS

Please refer to Ordinance No. 348.4947/50, Article XVIIa SP Zone (Specific Plan).

3.3.3 PLANNING STANDARDS

1. Site plan concepts for residential development within Village 3 are depicted on Exhibit 3-7, Village 3 Development Standards.
2. Primary access to Village 3 shall be provided via Street A and private streets.
3. Trails and sidewalks shall be provided as illustrated in Exhibit 4-20, Walkway Detail.
4. Walls and fencing located within Emerald Acres shall be constructed as delineated on Exhibit 4-18, Conceptual Wall and Fence Plan. Access to open space will be restricted to view fencing.
5. Public roadway landscape treatments, as shown in Exhibit 4-3, Stetson Avenue, Street 'A' and Street 'E' Street Sections are planned along California Avenue.
6. Private roadway landscape treatments, as shown in Exhibit 4-4, Private Local Streets and Alley Street Sections are planned along Local Streets and Private Drives.
7. Exhibit 4-1, Conceptual Landscape Key Map, depicts the location of edge treatments between Village 3 and open space areas. See Exhibit 4-15, Residential/Manufactured Slope/Detention Basin Edge Condition, for detailed standards on these edge conditions.
8. Edge treatments between Village 3 and Street A shall be provided. See Exhibit 4-14, Street A/Residential Edge Condition, for detailed standards on these edge conditions.
9. Neighborhood entry monuments may be provided at the entrances along Street A as illustrated on Exhibit 4-6, Primary and Neighborhood Entry.
10. Roundabout treatments shall be provided along Street A as illustrated on Exhibit 4-7, Roundabout Detail.
11. Fire Fuel Modification Zone edge conditions shall be established as illustrated on Exhibit 4-17, Residential/Open Space Edge Condition with 100' Fire Fuel Modification Zone.

12. Where manufactured slopes occur, criteria will follow slope guidelines within Section 4.2.9, Manufactured Slopes.
13. Please refer to Section 4, Design Guidelines for other related design criteria.
14. Please refer to Section 2, Specific Plan, for the following standards that apply site-wide:
 - 2-1. Land Use Plan
 - 2-2. Circulation Plan
 - 2-3. Drainage Plan
 - 2-4. Water and Sewer Plan
 - 2-5. Open Space and Recreation Plan
 - 2-6. Grading Plan
 - 2-7. Maintenance Plan



LAND USE SUMMARY	
PA-8	3.7 Acres
Medium Density Residential (5,000 SF Lots)	19 Units
	5.0 DU/AC
PA-9	6.6 Acres
Open Space-Recreation (Neighborhood Park)	
PA-10	2.8 Acres
Open Space-Conservation (Slopes)	
PA-11	2.8 Acres
Open Space-Conservation (WQMP Basin)	
PA-17 (Portion) Backbone Roads	5.1 Acres
Total	21.1 Acres

LEGEND	
	Project Boundary
	Village 2 Boundary
	Medium Density Residential (5,000 SF Lots)
	Open Space-Conservation (WQMP Basin)
	Open Space-Conservation (Slopes)
	Open Space-Recreation (Neighborhood Park)

EXHIBIT 3-6. VILLAGE 3 SUMMARY



CONCEPTUAL PRODUCT LAYOUT

RESIDENTIAL DEVELOPMENT STANDARDS-VILLAGE 3

Product Type - Single Family Detached	
Maximum Village Density	5.0 du/ac
Maximum Lot Area	5,000 sq.ft.
Minimum Lot Dimensions	50'X100'
Setbacks	
Minimum Front Yard (Back of Sidewalk to Building)	10'
Minimum Corner Side Yard (Back of Sidewalk to Building)	10'
Minimum Interior Side Yard	5'
Minimum Rear Yard	10'
Minimum Front Garage (Back of Sidewalk to Building)	20'
Minimum Side Garage (Back of Sidewalk to Building)	10'
Other	
Maximum Structural Height	35'
Maximum Lot Coverage	65%
Encroachments (Fireplaces, AC Units, Media Centers)	2'
Encroachments (Balconies, Porches, Decks)	5'

KEY MAP

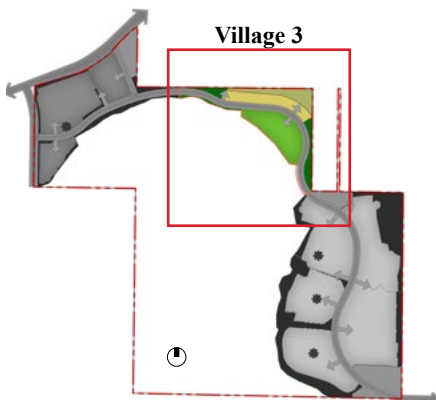


EXHIBIT 3-7. VILLAGE 3 DEVELOPMENT STANDARDS

3.4. VILLAGE 4 – HIGH DENSITY RESIDENTIAL

3.4.1 DESCRIPTIVE SUMMARY

Village 4, as shown on Exhibit 3-8, Village 4 Summary, consists of two high density residential neighborhoods (12A and 12B) and Commercial Retail (PA 13) located in the northwestern portion of the Emerald Acres community. Village 4 is bisected by Street A and is accessible via Street A and at a shared entrance along Street A that also serves PA 13. The northern boundary of Village 4 is adjacent to Florida Avenue, and the western boundary is adjacent to Calvert Avenue. A WQMP Basin (PA 14) is located along the eastern boundary and the southern boundary is adjacent to PA 16, which is the Natural Open Space parcel of the site.

As shown on Exhibit 3-9, Village 4 Development Standards –Attached Motorcourts, and Exhibit 3-10, Village 4 Development Standards – Detached Motorcourts, Village 4 provides for the development of up to two types of high density for-rent development. Attached residences arranged in motorcourt configuration and/or detached homes in a traditional neighborhood configuration fronting on private streets. Residences will be developed as for-rent products. Village 4 may be developed from a selection of motorcourt configurations ranging from a minimum of 4 units to a maximum of 8 units per attached motorcourt (see Exhibit 3-9 for example of 8-unit motorcourts). The motorcourt product type features attached residences arranged around a central motorcourt, where the garages for each residence are located. A lane provides shared access to the central motorcourt from the adjacent roadway. The frontages for each individual motorcourt unit are located along the outer perimeter of the building, facing outward. The detached home product is oriented with the garage facing the private street, in a traditional neighborhood configuration.

Overall, Village 4 will consist of a maximum of 123 dwelling units within 13.5 acres of the Emerald Acres property, allowing a total residential density of 9.1 du/ac.

Due to the wildland fire risk associated with residential land uses adjacent to natural open space, the implementation of Fire Fuel Modification Zones is required at the interface between residential uses within Village 4 and natural open space within Planning Area 16. Fire Fuel Modification Zone requirements are depicted on Exhibit 4-17, Residential/Open Space Edge Condition with 100' Fire Fuel Modification Zone, and generally require a reduction of vegetation within a 100' zone within the manufactured slope areas adjacent to residential uses that serves to reduce wildland fire risk to residential structures.

3.4.2 LAND USE AND DEVELOPMENT STANDARDS

Please refer to Ordinance No. 348.4947/50, Article XVIIa SP Zone (Specific Plan).

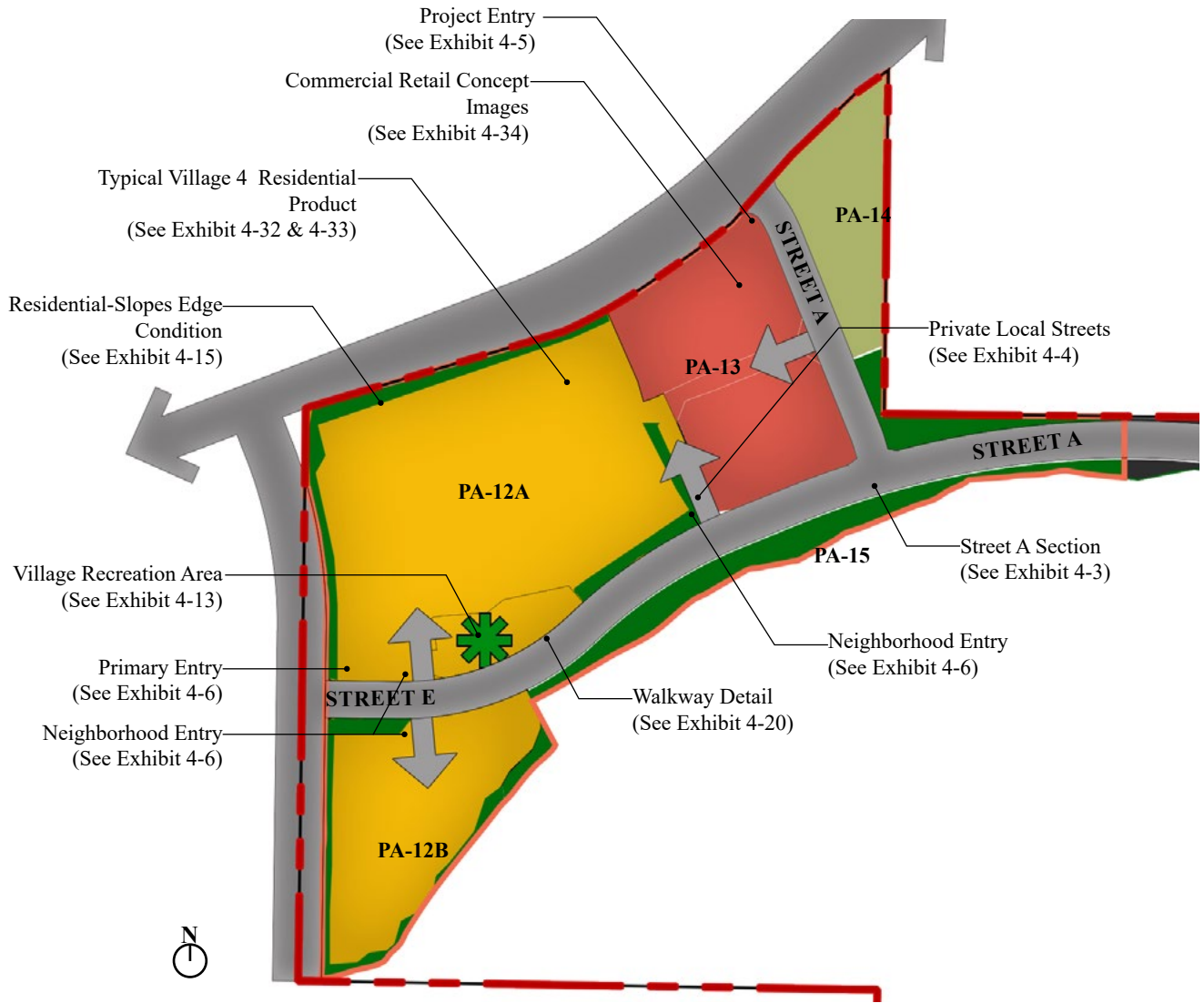
3.4.3 PLANNING STANDARDS

1. Site plan concepts for residential development within Village 4 are depicted on Exhibit 3-9, Village 4 Development Standards – Attached Motorcourts, and Exhibit 3-10, Village 4 Development Standards – Detached Motorcourts.
2. Unique housing products are provided as an option in Village 4 as illustrated by Exhibit 4-32 and 4-33, Village 4 Typical Product Elevations.
3. For the Motorcourt configuration, garages for each residence shall be accessed from a central interior motorcourt via a shared entry lane.
4. For the Motorcourt configuration, all residential entrances shall front outward along the outside of the building.

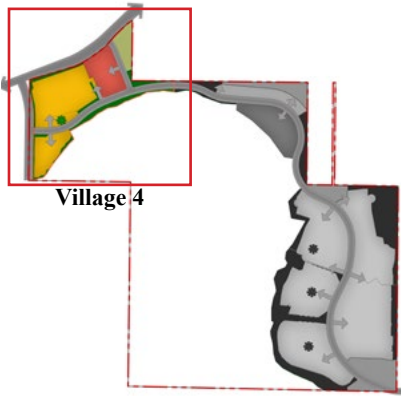
5. For the Motorcourt configuration, a 30' minimum setback is required between motorcourt buildings (building-to-building setback).
6. For the Motorcourt configuration, entry lanes shall include a minimum width of 24' with an accompanying 3'-wide landscaping planter on each side of the lane. (see Exhibit 3-9 and 3-10 for additional details).
7. Motorcourts shall require a minimum 30' distance between garages (garage-to-garage). (see Exhibit 3-9 and 3-10 for additional details).
8. Motorcourt buildings shall be set back from roadways by a minimum of 10', with encroachments of up to 5' permitted for balconies, porches and decks.
9. Landscaping planters are permitted to extend into the motorcourt between each garage. Garage doors may be recessed from the motorcourt by a minimum of 3' and a maximum of 5'.
10. Off street parking for motorcourts shall be provided at the following standards: 1.15 spaces per each 1-bedroom or studio unit, 2.25 spaces per each 2-bedroom unit, and 2.75 spaces per each 3-bedroom unit.
11. Motorcourt buildings shall have a maximum structural height of 42'.
12. For the Motorcourt configuration, residences may be developed with a minimum of 4 and a maximum of 8 units arranged in attached motorcourt configuration, however, all applicable development standards are to remain as shown in Exhibit 3-9.
13. Primary access to Village 4 will be provided from Street A.
14. Trails and sidewalks shall be provided as illustrated in Exhibit 4-20, Walkway Detail.
15. Walls and fencing located within Emerald Acres shall be constructed as delineated on Exhibit 4-18, Conceptual Wall and Fence Plan. Access to open space will be restricted by using view fencing.
16. Public roadway landscape treatments, as shown in Exhibit 4-3, Stetson Ave and Street A Street Sections are planned along Street A.
17. Private roadway landscape treatments, as shown in Exhibit 4-4, Private Local Streets and Alley Street Sections shall be implemented along Local Streets and Private Drives.
18. Public roadway treatments, as shown in Exhibit 4-2, Florida Ave and Calvert Ave Street Sections, are planned along Calvert Avenue.
19. Primary entry monument may be provided at the intersection of Street A and Calvert Avenue, as illustrated on Exhibit 4-6, Primary and Neighborhood Entry.
20. Exhibit 4-1, Conceptual Landscape Key Map, depicts the location of various edge treatments between Village 4 and Street A. See Exhibit 4-14, Street A/Residential Edge Condition, for detailed standards on these edge conditions.
21. Fire Fuel Modification Zone edge conditions shall be established as illustrated on Exhibit 4-17, Residential/Open Space Edge Condition with 100' Fire Fuel Modification Zone.
22. Where manufactured slopes occur, criteria will follow slope guidelines within Section 4.2.9, Manufactured Slopes.
23. Please refer to Section 4, Design Guidelines for other related design criteria.

24. Please refer to Section 2, Specific Plan, for the following standards that apply site-wide:

- 2-1. Land Use Plan
- 2-2. Circulation Plan
- 2-3. Drainage Plan
- 2-4. Water and Sewer Plan
- 2-5. Open Space and Recreation Plan
- 2-6. Grading Plan
- 2-7. Maintenance Plan



KEY MAP



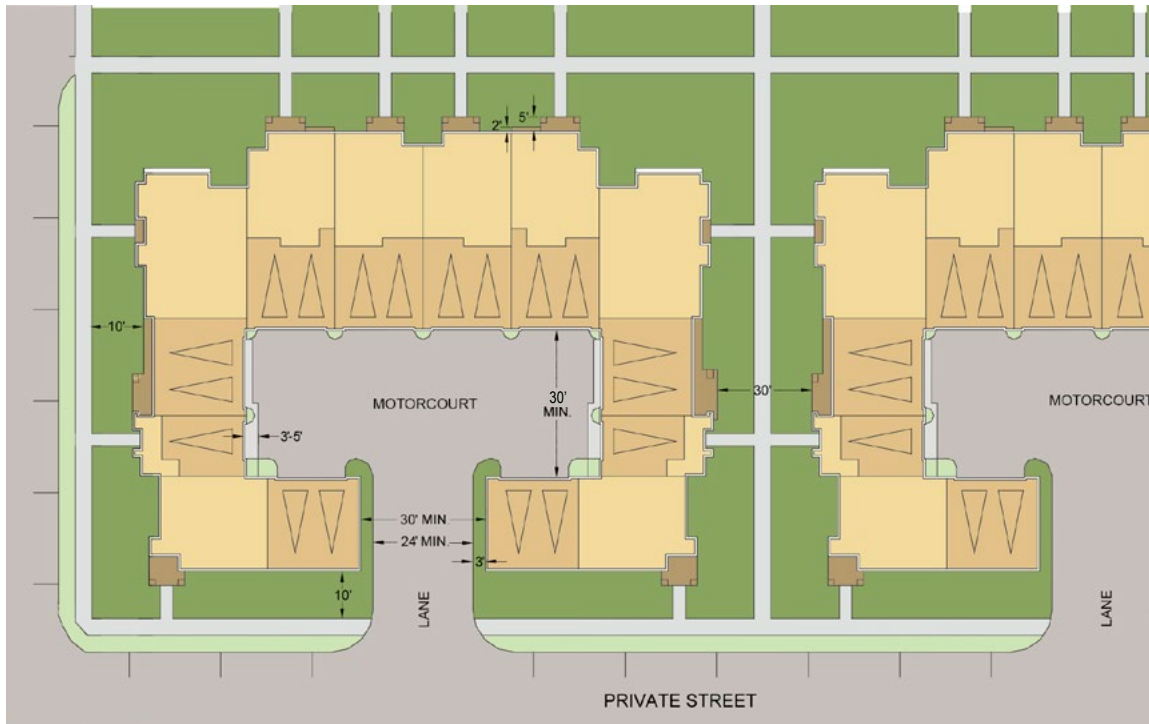
LAND USE SUMMARY

PA-12A & 12B	13.5 Acres
High Density Residential (Attached & Detached Motorcourt)	123 Units 9.1 DU/AC
PA-13	5.0 Acres
Commercial Retail	
PA-14	2.8 Acres
Open Space-Conservation (WQMP Basin)	
PA-14	6.8 Acres
Open Space-Conservation (Slopes)	
PA-17 (Portion) Backbone Roads	4.8 Acres
Total	32.9 Acres

LEGEND

- Project Boundary
- Village 2 Boundary
- High Density Residential (Attached & Detached Motorcourt)
- Commercial Retail
- Village Recreation Area
- Open Space-Conservation (Slopes)
- Open Space-Conservation (WQMP Basin)

EXHIBIT 3-8. VILLAGE 4 SUMMARY



CONCEPTUAL PRODUCT LAYOUT

RESIDENTIAL DEVELOPMENT STANDARDS-VILLAGE 4

Product Type - Attached Motorcourt	
Maximum Village Density	14.0 du/ac
Setbacks	
Minimum Front Yard (Back of Sidewalk to Building)	10'
Minimum Drive Aisle to Building	3'
Minimum Garage to Drive Aisle	3'
Minimum Side/Rear Garage (Back of Sidewalk to Building)	10'
Minimum Building Separation (Across Drive Aisle)	30'
Minimum Building Separation (Side to Side)	30'
Other	
Maximum Structural Height	42'
Maximum Lot Coverage	60%
Encroachments (Fireplaces, AC Units, Media Centers)	2'
Encroachments (Balconies, Porches, Decks)	5'
Parking Per Unit	
1 Bedroom or Studio Unit	1.15 Spaces
2 Bedroom	2.25 Spaces
3 Bedroom	2.75 Spaces

KEY MAP

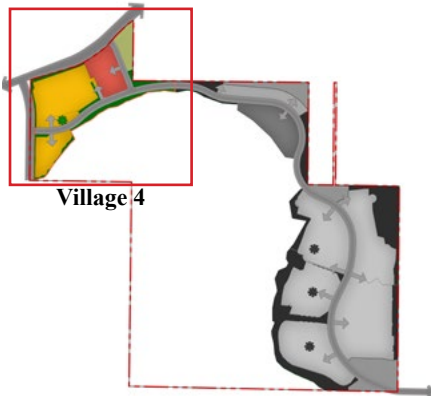


EXHIBIT 3-9. VILLAGE 4 DEVELOPMENT STANDARDS - ATTACHED MOTORCOURTS



CONCEPTUAL PRODUCT LAYOUT

RESIDENTIAL DEVELOPMENT STANDARDS-VILLAGE 4

Product Type - Detached Motorcourt	
Maximum Village Density	14.0 du/ac
Setbacks	
Minimum Front Yard (Back of Sidewalk to Building)	10'
Minimum Drive Aisle to Building	3'
Minimum Interior Side Yard	5'
Minimum Rear Yard	10'
Minimum Garage to Drive Aisle	3'
Minimum Side Garage (Back of Sidewalk to Building)	10'
Minimum Building Separation (Across Drive Aisle)	30'
Minimum Building Separation (Side to Side)	10'
Other	
Maximum Structural Height	42'
Maximum Lot Coverage	60%
Encroachments (Fireplaces, AC Units, Media Centers)	2'
Encroachments (Balconies, Porches, Decks)	5'
Parking Per Unit	
1 Bedroom or Studio Unit	1.15 Spaces
2 Bedroom	2.25 Spaces
3 Bedroom	2.75 Spaces

KEY MAP

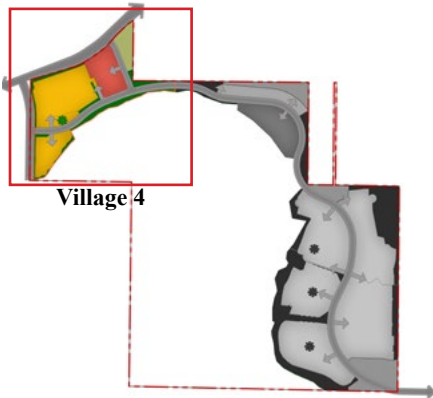


EXHIBIT 3-10. VILLAGE 4 DEVELOPMENT STANDARDS - DETACHED MOTORCOURTS

3.5. VILLAGE 4-PLANNING AREA 13 – COMMERCIAL/RETAIL

3.5.1 DESCRIPTIVE SUMMARY

Planning Area (PA) 13 in Village 4, as shown on Exhibit 3-11, Planning Area 13 Summary, is a retail commercial Planning Area located in the northernmost portion of Emerald Acres, at the northwest corner of Street A and Street E, adjacent to Florida Avenue. Planning Area 13 is accessible via an entrance along Street E and at a shared access point from Street A. No direct access from Florida Avenue is provided to Planning Area 13. The western boundary of PA 13 is directly adjacent to High Density Residential in Village 4, the eastern boundary is adjacent to PA 14, and the southern boundary is adjacent to Street A across from PA 16. PA 13 accommodates a variety potential retail uses that would serve the residents of Emerald Acres and the surrounding communities.

Overall, and as shown on Exhibit 3-12, Planning Area 13 Development Standards, PA 13 provides for the development of commercial uses within the 5.0-acre portion of the Emerald Acres community.

3.5.2 LAND USE AND DEVELOPMENT STANDARDS

Please refer to Ordinance No. 348.4947/50, Article XVIIa SP Zone (Specific Plan).

3.5.3 PLANNING STANDARDS

1. Site plan concepts and site planning guidelines for commercial development within Planning Area 13 are depicted on Exhibit 3-12, Planning Area 13 Development Standards. These site planning guidelines are intended to allow flexibility to accommodate future commercial development within Planning Area 13, while providing for a high-quality retail development.
2. Primary access to Planning Area 5 shall be provided from Street E and Street A.
3. Walls and fencing located within Emerald Acres shall be constructed as delineated on Exhibit 4-18, Conceptual Wall and Fence Plan. Access to open space will be restricted by using view fencing.
4. Public roadway landscape treatments, as shown in Exhibit 4-2, Florida Ave and Calvert Ave Street Sections are planned along Florida Avenue.
5. Public roadway landscape treatments, as shown in Exhibit 4-3, Stetson Avenue, Streets 'A' and 'E' Street Sections are planned along Street A.
6. Private roadway landscape treatments, as shown in Exhibit 4-4, Private Local Streets and Alley Street Sections are planned along Street E.
7. Commercial signs are permitted within Planning Area 5 in accordance with all applicable County of Riverside signage requirements.
8. Trails and sidewalks will be provided as illustrated in Exhibit 4-20, Walkway Detail.
9. Where manufactured slopes occur, criteria will follow slope guidelines within Section 4.2.9, Manufactured Slopes.
10. Please refer to Section 4, Design Guidelines for other related design criteria.

11. Please refer to Section 2, Specific Plan, for the following standards that apply site-wide:

2-1. Land Use Plan

2-2. Circulation Plan

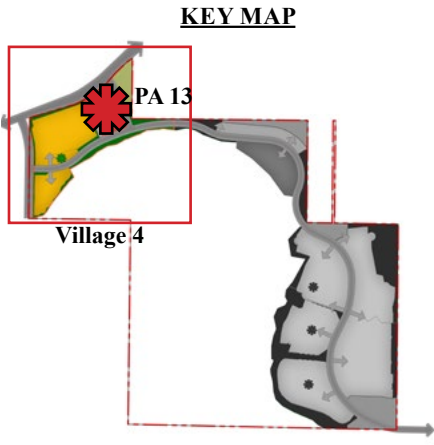
2-3. Drainage Plan

2-4. Water and Sewer Plan

2-5. Open Space and Recreation Plan

2-6. Grading Plan

2-7. Maintenance Plan



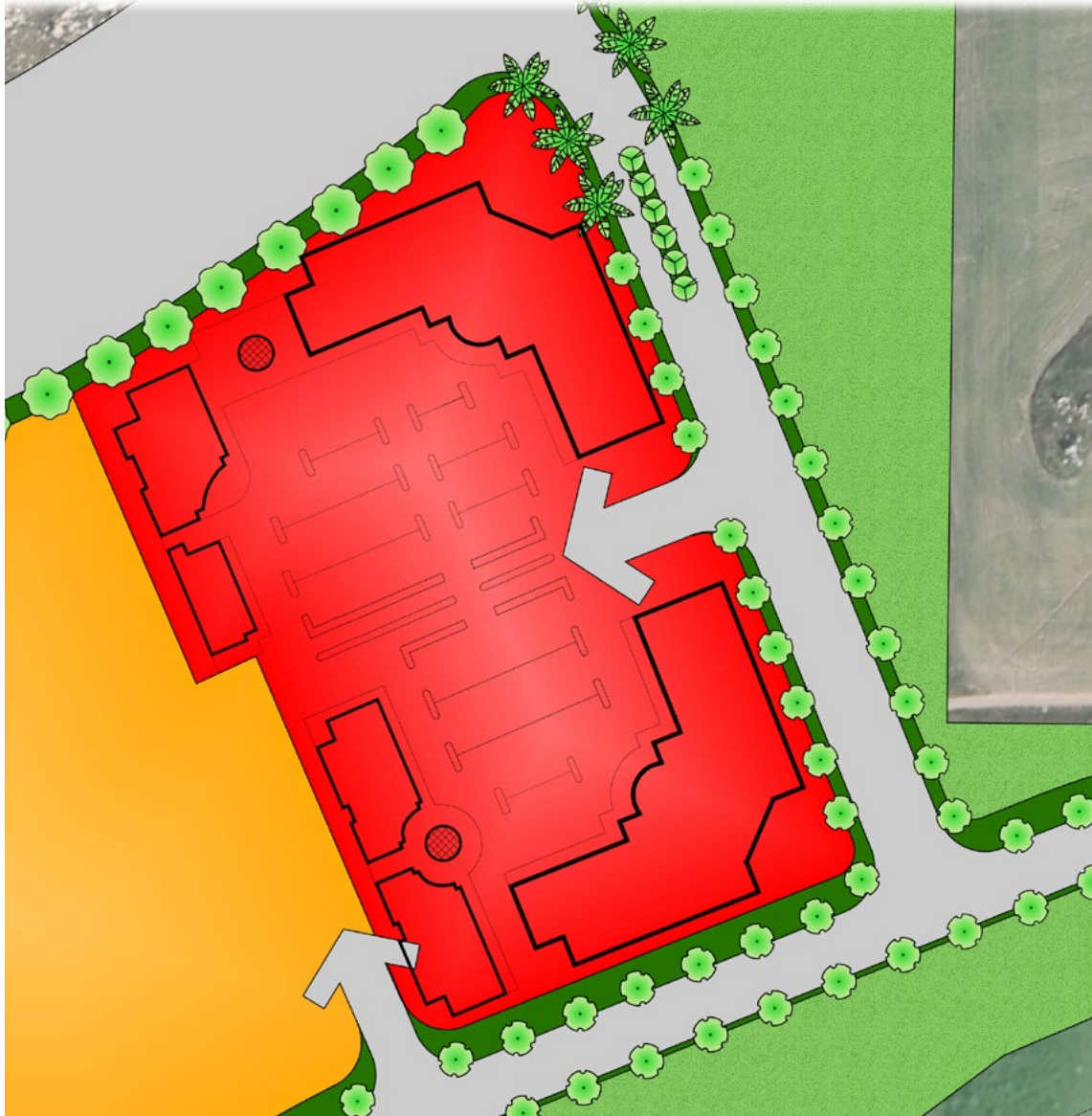
LAND USE SUMMARY

PA-13 Commercial/Retail	5.0 Acres
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LEGEND

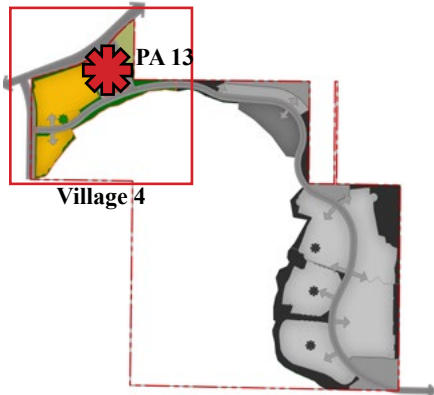
	Project Boundary
	Village 4 Boundary
	Commercial/Retail

EXHIBIT 3-11. PLANNING AREA 13 SUMMARY



CONCEPTUAL SITE LAYOUT

KEY MAP



DEVELOPMENT STANDARDS-VILLAGE 4- PA 13

Commercial/Retail	
Maximum Lot Coverage	25% F.A.R.
Setbacks	
Minimum Front Setback (Back of Sidewalk to Building)	10'
Minimum Side Setback	10'
Minimum Street Side Setback	10'
Minimum Rear Setback	15'
Other	
Maximum Building Height	35'

EXHIBIT 3-12. PLANNING AREA 13 DEVELOPMENT STANDARDS

3.6. VILLAGE 1-PLANNING AREA 4 – PUBLIC FACILITIES

3.6.1 DESCRIPTIVE SUMMARY

Planning Area 4, as shown on Exhibit 3-1, Village 1 Summary, is the southernmost Planning Area within the Emerald Acres property. Planning Area 6 comprises 0.9 acres and is generally south of Village 1 and is directly adjacent to the southern boundary of PA 3.

Planning Area 4 is designed for use by a water pump station and is integral to the Emerald Acres master water plan.

3.6.2 LAND USE AND DEVELOPMENT STANDARDS

Please refer to Ordinance No. 348.4947/50, Article XVIIa SP Zone (Specific Plan).

3.6.3 PLANNING STANDARDS

1. A water pump station and appurtenant infrastructure facilities is permitted within Planning Area 16.
2. Please refer to Section 2, Specific Plan, for the following standards that apply site-wide:
 - 2-1. Land Use Plan
 - 2-2. Circulation Plan
 - 2-3. Drainage Plan
 - 2-4. Water and Sewer Plan
 - 2-5. Open Space and Recreation Plan
 - 2-6. Grading Plan
 - 2-7. Maintenance Plan

3.7. PLANNING AREA 16 – OPEN SPACE/CONSERVATION HABITAT

3.7.1 DESCRIPTIVE SUMMARY

Approximately 191.3 acres of land is designated for Open Space – Conservation Habitat within Emerald Acres. As shown on Exhibit 3-13, Planning Area 16 Summary, Planning Area 16 comprises 191.3 acres of land within the southwestern portion of the Emerald Acres property.

This Open Space – Conservation Habitat planning area is to remain undisturbed, thus preserving several natural features of the site including, but not limited to: hillside areas with 25% or greater slope, natural drainage courses, native vegetation, and rock outcroppings. Access to PA 16 is restricted through the use of view fencing, with the exception of an equestrian access to PA 16 that occurs south of the Village 1 boundary in PA 2 (between PA 1B and PA 4). This access maintains the historical use of PA 16 for local equestrians. Planning Area 16 may be dedicated to an Open Space management entity for long-term ownership.

3.7.2 LAND USE AND DEVELOPMENT STANDARDS

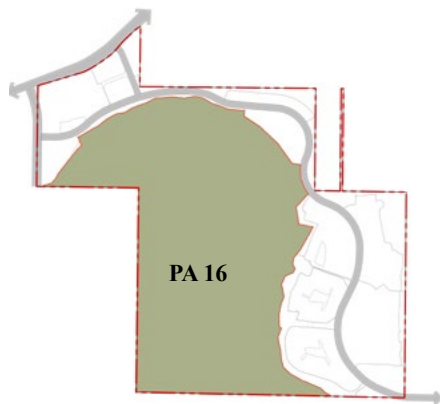
Please refer to Ordinance No. 348.4947/50, Article XVIIa SP Zone (Specific Plan).

3.7.3 PLANNING STANDARDS

1. Fire Fuel Modification Zone edge conditions shall be established as illustrated on Exhibit 4-17, Residential/Open Space Edge Condition with 100' Fire Fuel Modification Zone.
2. Walls and fencing located within Emerald Acres shall be constructed as delineated on Exhibit 4-18, Conceptual Wall and Fence Plan. Access to open space will be restricted by using view fencing, except where equestrian access is provided south of Village 1.
3. Please refer to Section 2, Specific Plan, for the following standards that apply site-wide:
 - 2-1. Land Use Plan
 - 2-2. Circulation Plan
 - 2-3. Drainage Plan
 - 2-4. Water and Sewer Plan
 - 2-5. Open Space and Recreation Plan
 - 2-6. Grading Plan
 - 2-7. Maintenance Plan



KEY MAP



LAND USE SUMMARY

PA-16 Open Space-Conservation Habitat	191.3 Acres
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LEGEND




-  Project Boundary
-  PA 16 Boundary
-  Open Space-Conservation Habitat

EXHIBIT 3-13. PLANNING AREA 16 SUMMARY

3.8. VILLAGE 1-PLANNING AREAS 2 AND 3 – OPEN SPACE/CONSERVATION

3.8.1 DESCRIPTIVE SUMMARY

Planning Areas 2 and 3, as shown on Exhibit 3-1, Village 1 Summary, are generally located within the southern portion of the Emerald Acres property and provide a total of 14.3 acres of open space.

Planning Areas 2 and 3 consist primarily of manufactured slopes that serve the residents of Village 1 by providing landscaping, stormwater percolation/WQMP basins, fire fuel modification zones and buffers between varying land uses. PA 3, the parcel with WQMP Basin is 3.7 acres and includes trails that allow formal pedestrian mobility and other recreational uses.

3.8.2 LAND USE AND DEVELOPMENT STANDARDS

Please refer to Ordinance No. 348.4947/50, Article XVIIa SP Zone (Specific Plan).

3.8.3 PLANNING STANDARDS

1. Access to Planning Areas 2 and 3 is provided by the Multi-Use Trail along Street A and adjacent village sidewalks.
2. An Equestrian Trail access point shall be included within PA 2 along Street A.
3. A trail shall be provided within PA 3 within the area east of Village 1 as shown on Exhibit 3-1, Village 1 Summary and as illustrated in Exhibit 4-20, Walkway Detail.
4. Walls and fencing located within Emerald Acres shall be constructed as delineated on Exhibit 4-18, Conceptual Wall and Fence Plan. Access to open space shall be restricted by using view fencing.
5. Exhibit 4-1, Conceptual Landscape Key Map, depicts the location of edge treatments between Village 1 residential Planning Areas 1A, 1B and 1C and Planning Areas 2 and 3. See Exhibit 4-15, Residential/Manufactured Slope/Detention Basin Edge Condition, for detailed standards on these edge conditions.
6. Fire Fuel Modification Zone edge conditions shall be established within manufactured slopes, as illustrated on Exhibit 4-17, Residential/Open Space Edge Condition with 100' Fire Fuel Modification Zone.
7. Where manufactured slopes occur, criteria will follow slope guidelines within Section 4.2.9, Manufactured Slopes.
8. Please refer to Section 4, Design Guidelines for other related design criteria.
9. Please refer to Section 2, Specific Plan, for the following standards that apply site-wide:
 - 2-1. Land Use Plan
 - 2-2. Circulation Plan
 - 2-3. Drainage Plan
 - 2-4. Water and Sewer Plan
 - 2-5. Open Space and Recreation Plan
 - 2-6. Grading Plan
 - 2-7. Maintenance Plan

3.9. VILLAGE 2-PLANNING AREAS 6 AND 7 – OPEN SPACE/CONSERVATION

3.9.1 DESCRIPTIVE SUMMARY

Planning Areas 6 and 7, as shown on Exhibit 3-3, Village 2 Summary, are generally located within the eastern, central portion of the Emerald Acres community and provide a total of 9.5 acres of open space.

Planning Areas 6 and 7 consist primarily of manufactured slopes and which serve the residents of Village 2 by providing landscaping, storm water percolation/WQMP basins, fire fuel modification zones and buffers between varying land uses. PA 7 contains the WQMP Basin with an area of 1.7 acres.

3.9.2 LAND USE AND DEVELOPMENT STANDARDS

Please refer to Ordinance No. 348.4947/50, Article XVIIa SP Zone (Specific Plan).

3.9.3 PLANNING STANDARDS

1. Access to Planning Areas 6 and 7 is provided by the Multi-Use Trail along Street A and adjacent village sidewalks.
2. Walls and fencing located within Emerald Acres shall be constructed as delineated on Exhibit 4-18, Conceptual Wall and Fence Plan. Access to open space will be restricted by using view fencing.
3. Exhibit 4-1, Conceptual Landscape Key Map, depicts the location of edge treatments between Village 2 residential Planning Areas 5A and 5B and Planning Areas 6 and 7. See Exhibit 4-15, Residential/Manufactured Slope/Detention Basin Edge Condition, for detailed standards on these edge conditions.
4. Fire Fuel Modification Zone edge conditions shall be established as illustrated on Exhibit 4-17, Residential/Open Space Edge Condition with 100' Fire Fuel Modification Zone.
5. Where manufactured slopes occur, criteria will follow slope guidelines within Section 4.2.9, Manufactured Slopes.
6. Please refer to Section 4, Design Guidelines for other related design criteria.
7. Please refer to Section 2, Specific Plan, for the following standards that apply site-wide:
 - 2-1. Land Use Plan
 - 2-2. Circulation Plan
 - 2-3. Drainage Plan
 - 2-4. Water and Sewer Plan
 - 2-5. Open Space and Recreation Plan
 - 2-6. Grading Plan
 - 2-7. Maintenance Plan

3.10. VILLAGE 3-PLANNING AREAS 10 AND 11 – OPEN SPACE/CONSERVATION**3.10.1 DESCRIPTIVE SUMMARY**

Planning Areas 10 and 11, as shown on Exhibit 3-6, Village 3 Summary, are generally located within the northeastern portion of the Emerald Acres property and provide a total of 5.6 acres of open space.

Planning Areas 10 and 11 consist primarily of manufactured slope areas which serve the residents of Village 3 by providing landscaping, storm water percolation/WQMP basins, Fire Fuel Modification Zones and buffers between varying land uses. PA 11 contains the WQMP basin and has an areas of 2.8 acres.

3.10.2 LAND USE AND DEVELOPMENT STANDARDS

Please refer to Ordinance No. 348.4947/50, Article XVIIa SP Zone (Specific Plan).

3.10.3 PLANNING STANDARDS

1. Access to Planning Areas 10 and 11 is provided by the Multi-Use Trail along Street A and adjacent village sidewalks.
2. Walls and fencing located within Emerald Acres shall be constructed as delineated on Exhibit 4-18, Conceptual Wall and Fence Plan. Access to open space will be restricted by using view fencing.
3. Exhibit 4-1, Conceptual Landscape Key Map, depicts the location of edge treatments between Village 3 residential Planning Area 8 and Planning Areas 10 and 11. See Exhibit 4-15, Residential/Manufactured Slope/Detention Basin Edge Condition, for detailed standards on these edge conditions.
4. Fire Fuel Modification Zone edge conditions shall be established as illustrated on Exhibit 4-17, Residential/Open Space Edge Condition with 100' Fire Fuel Modification Zone.
5. Where manufactured slopes occur, criteria will follow slope guidelines within Section 4.2.9, Manufactured Slopes.
6. Please refer to Section 4, Design Guidelines for other related design criteria.
7. Please refer to Section 2, Specific Plan, for the following standards that apply site-wide:
 - 2-1. Land Use Plan
 - 2-2. Circulation Plan
 - 2-3. Drainage Plan
 - 2-4. Water and Sewer Plan
 - 2-5. Open Space and Recreation Plan
 - 2-6. Grading Plan
 - 2-7. Maintenance Plan

3.11. VILLAGE 4-PLANNING AREAS 14 AND 15 – OPEN SPACE / CONSERVATION

3.11.1 DESCRIPTIVE SUMMARY

Planning Areas 14 and 15, as shown on Exhibit 3-8, Village 4 Summary, and Exhibit 3-11, Planning Area 13 Summary, are generally located within the northwestern portion of the Emerald Acres property and provide a total of 9.6 acres of open space.

Planning Areas 14 and 15, consist primarily of manufactured slopes with aesthetic value which serve the residents of Village 4 by providing landscaping, storm water percolation/WQMP basins, public facilities, fire fuel modification zones and buffers between varying land uses. PA 14 is 2.8 acres and contains the WQMP Basin, adjacent to Commercial Retail PA 13.

3.11.2 LAND USE AND DEVELOPMENT STANDARDS

Please refer to Ordinance No. 348.4947/50, Article XVIIa SP Zone (Specific Plan).

3.11.3 PLANNING STANDARDS

1. Access to Planning Areas 14 and 15, is provided by the Multi-Use Trail along Street A and adjacent village sidewalks.
2. Walls and fencing located within Emerald Acres shall be constructed as delineated on Exhibit 4-18, Conceptual Wall and Fence Plan. Access to open space will be restricted by using view fencing.
3. Exhibit 4-1, Conceptual Landscape Key Map, depicts the location of edge treatments between Village 4 residential and retail and Planning Area 15. See Exhibit 4-15, Residential/Manufactured Slope/ Detention Basin Edge Condition, for detailed standards on these edge conditions.
4. A water pump station and appurtenant facilities shall be permitted within Planning Area 14.
5. Fire Fuel Modification Zone edge conditions shall be established as illustrated on Exhibit 4-17, Residential/Open Space Edge Condition with 100' Fire Fuel Modification Zone.
6. Where manufactured slopes occur, criteria will follow slope guidelines within Section 4.2.9, Manufactured Slopes.
7. Please refer to Section 4, Design Guidelines for other related design criteria.
8. Please refer to Section 2, Specific Plan, for the following standards that apply site-wide:
 - 2-1. Land Use Plan
 - 2-2. Circulation Plan
 - 2-3. Drainage Plan
 - 2-4. Water and Sewer Plan
 - 2-5. Open Space and Recreation Plan
 - 2-6. Grading Plan
 - 2-7. Maintenance Plan

3.12. VILLAGE 3-PLANNING AREA 9 – NEIGHBORHOOD PARK

3.12.1 DESCRIPTIVE SUMMARY

Planning Area 9, as shown on Exhibit 3-1, Village 1 Summary, and Exhibit 3-6, Village 3 Summary, is centrally located within the Emerald Acres property and provides for a 6.6-acre Neighborhood Park. Access to the Neighborhood Park is provided via Street A. PA 9 is adjacent to the Natural Open Space PA 16 and south of residential Planning Area 8.

The Neighborhood Park within Planning Area 9 provides residents of the Emerald Acres community and the public at large with recreational amenities that may include a basketball court, baseball field, soccer field, picnic area, tot lot, restroom facilities, and on-site parking.

3.12.2 LAND USE AND DEVELOPMENT STANDARDS

Please refer to Ordinance No. 348.4947/50, Article XVIIa SP Zone (Specific Plan).

3.12.3 PLANNING STANDARDS

1. Vehicular access to Planning Area 9 is provided by Street A.
2. A parking lot serving the Neighborhood Park shall be located centrally in Planning Area 9, accommodating a minimum of 30 parking spaces.
3. Pedestrian access to Planning Area 9 is provided by the Multi-Use Trail along Street A. Trails and sidewalks along Street A will be provided as illustrated in Exhibit 4-20, Walkway Detail.
4. Park amenities may be provided as according to Exhibit 4-9, Active Neighborhood Park.
5. Walls and fencing located within Emerald Acres shall be constructed as delineated on Exhibit 4-18, Conceptual Wall and Fence Details. Access to open space will be restricted by using view fencing.
6. Where manufactured slopes occur, criteria will follow slope guidelines within Section 4.2.9, Manufactured Slopes.
7. Please refer to Section 4, Design Guidelines for other related design criteria.
8. Please refer to Section 2, Specific Plan, for the following standards that apply site-wide:
 - 2-1. Land Use Plan
 - 2-2. Circulation Plan
 - 2-3. Drainage Plan
 - 2-4. Water and Sewer Plan
 - 2-5. Open Space and Recreation Plan
 - 2-6. Grading Plan
 - 2-7. Maintenance Plan

SECTION 4: DESIGN GUIDELINES

4.1. PURPOSE AND INTENT

Emerald Acres is a 335.5-acre community which maximizes the benefits of the distinguishable qualities of its location and varied topography. The Emerald Acres Specific Plan has been influenced by, and is sensitive to, the environment in which it is located. The Design Guidelines for Emerald Acres distinguish the community and represent the overall framework which establishes the community's thematic identity, determines the aesthetic standards for the community recreational amenities, and forms the linkage between the neighborhoods and other land uses.

These Design Guidelines are intended to establish standards for the quality of development, through defining community character and providing an aesthetically pleasing and cohesive environment for the Emerald Acres community. More specifically, the objectives of the Design Guidelines are:

- Paramount to the purpose of the Emerald Acres Design Guidelines is to provide the County of Riverside with the necessary assurances that the Specific Plan area will develop in accordance with the quality and character proposed herein;
- To provide guidance to developers, builders, engineers, architects, landscape architects, and other professionals involved in the design and construction of Emerald Acres in order to maintain the desired design quality;
- To provide a framework for the formulation of Covenants, Conditions and Restrictions (CC&R's) for the Project's Homeowners' Association (HOA);
- To provide guidance in the formulation of concise development guidelines for the various planning areas within the Specific Plan boundaries; and
- To provide development guidelines which enable Emerald Acres to develop its own theme and character while allowing it to interface and respond to the natural and man-made fabric of adjacent areas.

The Design Guidelines provided herein are intended as a living document. They may be subject to modification over time to allow for response to unanticipated conditions, such as changes in trends, community desires, and the marketplace.

The Design Guidelines are divided into two principal components: Community Elements and Architectural Guidelines. Written descriptions and graphic exhibits within each section address the design of typical elements within the Emerald Acres community. Intended to complement one another, these components define the design concept, physical character, and theme of Emerald Acres, while ensuring land use compatibility. Together they combine to form a distinctive community offering a high-quality environment.

4.2. COMMUNITY ELEMENTS

Community elements are major community improvements that occur at the community or neighborhood level and which help to set the overall environmental design theme for this Emerald Acres Specific Plan. These major improvements or theme elements include:

- Streetscapes
- Landscaping
- Community Entries

- Project Signage
- Land Use Transitions
- Greenbelts, Parks, Trails and Open Space
- Community Walls and Fences

These community elements commonly occur throughout the community and unite the community under a common design vocabulary and theme. General design guidelines and criteria for the community theme elements are contained in the sections that follow.

4.2.1 COMMUNITY DESIGN THEMES

As demonstrated by the Conceptual Landscape Key Map (see Exhibit 4-1), careful thought has been given to integrate the structural and aesthetic elements of a balanced, active community. This community is a planned community that captures and enhances the unique character of the hillside setting in which it is located. This community offers its residents a living environment in which portions of the site are devoted to natural open space for conservation, recreational space, and trails. The Emerald Acres Specific Plan has been designed to focus both internally and externally: internally with the provision of residential and recreational space along with natural open-space and a greenbelt, and externally by maintaining a sense of context with the surrounding communities.

4.2.2 STREETSCAPES

A hierarchy of landscape treatments is provided for primary roadways within the community. To provide variety and to help define areas within this Emerald Acres Specific Plan, distinctive trees can be used in streetscape landscaping. Different parkway trees are suggested for entry roadways and for the private roadways within the community. Street trees for roadways within Emerald Acres may be selected subject to approval by the County of Riverside.

4.2.2.1 FLORIDA AVENUE STREETSCAPE

Northerly of Village 4, Florida Avenue is classified as a Modified Public Expressway with 60' of pavement in each direction with a 184' right-of-way overall. Florida Avenue is a Caltrans Capital Improvement Project (CIP). (See Exhibit 4-2, Florida Avenue and Calvert Avenue Street Sections)

4.2.2.2 CALVERT AVENUE STREETSCAPE

Westerly of Village 4, Calvert Avenue is classified as a Modified Secondary Highway with 76' of pavement within a 112' right-of-way. Calvert Avenue provides two vehicular lanes in each direction, a total of 10' for sidewalk and 26' for landscaping. The Emerald Acres Specific Plan will improve the east 68' of the 112' right-of-way (half-width improvements). The remaining 44' right-of-way will be improved by future development. (See Exhibit 4-2, Florida Avenue and Calvert Avenue Street Sections)

4.2.2.3 STREET A STREETSCAPE

At the entry to Village 4 from Calvert Avenue, Street A is classified as a public Modified Collector Road with 44' of pavement within a 74' right-of-way. Within this 74' section, Street A provides two vehicular lanes in each direction with 5' of sidewalk and 8' for parkway landscaping along the northbound side of the roadway, as well as a 10'-wide Multi-Use Trail. The southbound side of the roadway contains a 4' landscaped parkway. (See Exhibit 4-3, Stetson Avenue, Streets 'A' and 'E' Street Sections)

South of Village 3, Street A is modified to expand the right-of-way to 85'. Within this portion of the roadway, Street A provides two vehicular lanes in each direction with 5' sidewalks on each side of the road. On the south side of the street, the sidewalk is buffered from the roadway by a 6' landscaped parkway and from adjacent uses by a 4' landscape buffer. On the north side of the street, the sidewalk is buffered from the roadway by a 4' landscape parkway. In addition, the north side of the street contains a 10' wide Multi-Use Trail, which is buffered from the sidewalk by a 4' landscape buffer and from adjacent uses by a 3' landscape buffer. At the southeastern boundary of the Emerald Acres community, Street A transitions to Stetson Avenue. (See Exhibit 4-3, Stetson Avenue, Streets 'A' and 'E' Street Sections)

4.2.2.4 STREET E STREETSCAPE

Adjacent to the eastern boundary of Planning Area 13, Street E is classified as a Modified Collector Road with 49' of pavement and a 15' landscaped median within a 74' right-of-way. Street E provides two vehicular lanes in each direction with 5' of sidewalk and 10' of landscaped parkway on the southbound side of the street and 5' of sidewalk with 5' for parkway landscaping on the northbound side of the street. (See Exhibit 4-3, Stetson Avenue, Streets 'A' and 'E' Street Sections)

4.2.2.5 STETSON AVENUE STREETSCAPE

At the southeastern boundary of the Emerald Acres community, Street A transitions into Stetson Avenue. Off-site improvements will be made to Stetson Avenue from the terminus of Street A to the intersection of California Avenue and Stetson Avenue. Stetson Avenue improvements include 40' of pavement within a 63' right-of-way. At this section, Stetson Avenue provides one vehicular lane in each direction with = 4.5' of sidewalk and 5.5' of parkway on the southbound side of the road and 5.5' of sidewalk with 7.5' for parkway landscaping on the northbound side of the road. (See Exhibit 4-3, Stetson Avenue, Streets 'A' and 'E' Street Sections)

4.2.2.6 PRIVATE LOCAL STREETSCAPE

Private Local Streets are located within Villages 1 through 4, and may include one of four different streetscape designs: a 37', 41', 45', or a 61' streetscape. (See Exhibit 4-4, Private Local Streets and Alley Street Sections). The 37' and 41' roadway streetscapes apply to residential Private Roads within Villages 1 through 4 and include one vehicular lane in each direction, 4' of sidewalk on both sides of the road, and 4' of parkway on both sides of the road. The 45' roadway streetscape applies to Street J within Village 1 and includes one vehicular lane in each direction, 5' of sidewalk on both sides of the road, and 10' of parkway on both sides of the road. The 61' roadway streetscape applies to Streets B and E within Village 1 and includes one vehicular lane in each direction and a 14' landscaped median with 8' of sidewalk included on both sides of the road outside of the right-of-way.

4.2.2.7 ALLEY STREETSCAPE

Private Alleys include one vehicular lane in each direction, 25' of pavement, and 6' of landscaped parkway on each side of the alley. (See Exhibit 4-4, Private Local Streets and Alley Street Sections)

4.2.3 ENTRIES AND ENTRY TREATMENTS

To identify the Emerald Acres Specific Plan area and individual neighborhoods within the community, a hierarchy of entry statements are provided as described in the following text and exhibits. It is the intent of these entry statements to both identify the community, and to establish a theme consistent with the overall design concept for the community. Careful consideration has been given to the design of community entries. The design intent is two-fold: to create distinctive visual gateways into the community announcing



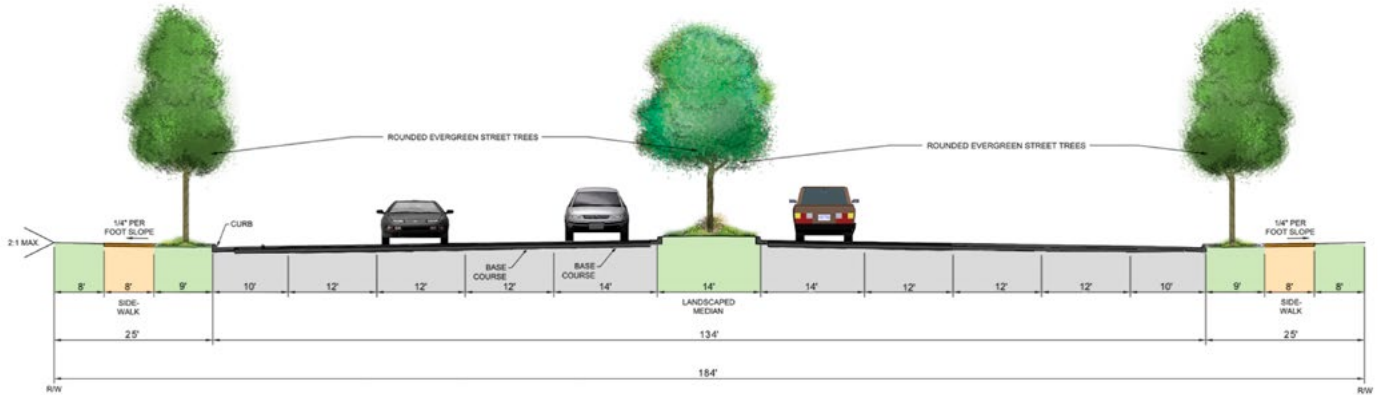
LEGEND

- STREET SECTIONS**
- A** Florida Avenue - 184' R.O.W.
 - B** Calvert Avenue - 112' R.O.W.
 - C** Street A - 85' R.O.W.
 - D** Street A - 74' R.O.W.
 - E** Street E - 74' R.O.W.
 - F** Stetson Avenue - 63' R.O.W.

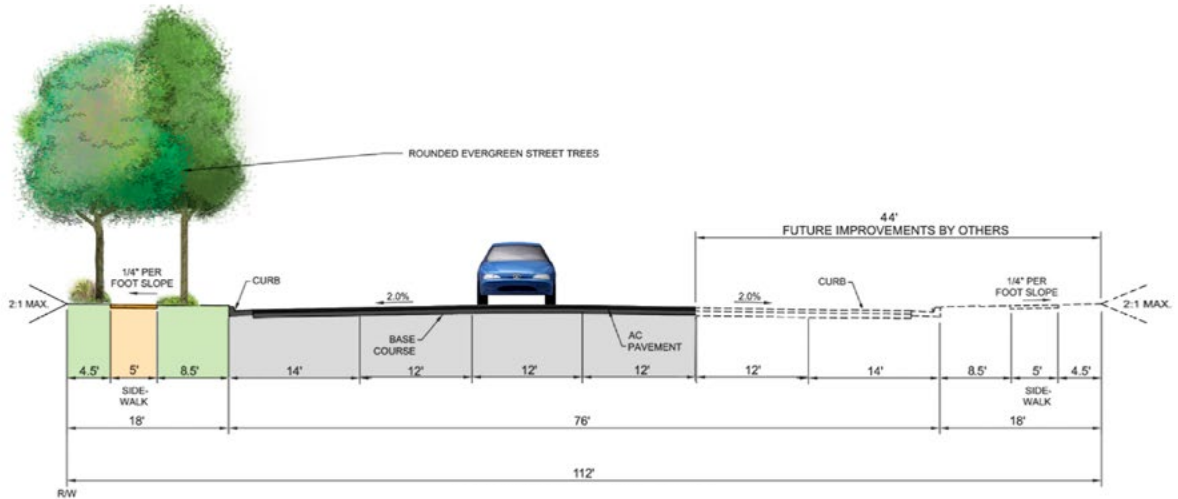
- EDGE CONDITIONS**
- F** Street A/Residential
 - G** Residential/Manufactured Slope/ Detention Basin
 - H** Residential/Off-site
 - I** Residential/Open Space with 100' Fire Fuel Modification Zone

- ENTRY FEATURES & RECREATION**
- K** Project Entry
 - L** Primary Entry
 - M** Neighborhood Entry
 - N** Roundabout Detail
 - O** Active Neighborhood Park
 - 100' Fire Fuel Modification Zone
 - Project Boundary

EXHIBIT 4-1. CONCEPTUAL LANDSCAPE KEY MAP

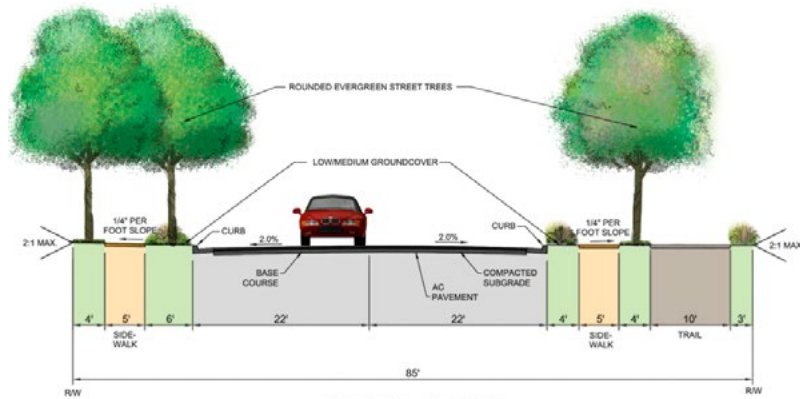


A
 HIGHWAY 74/ FLORIDA AVENUE
 184' R.O.W.

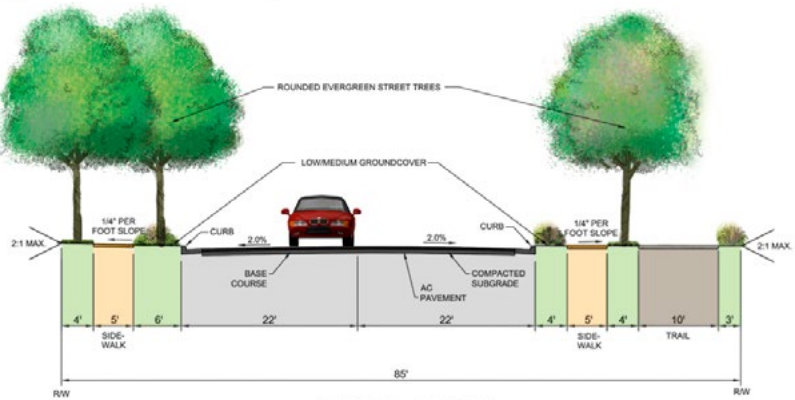


B
 CALVERT AVENUE
 112' R.O.W.

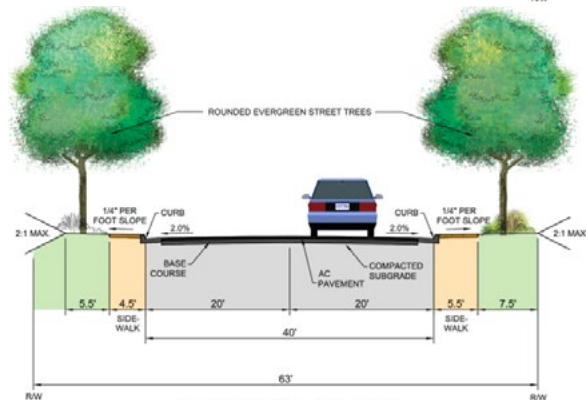
EXHIBIT 4-2. FLORIDA AVENUE AND CALVERT AVENUE STREET SECTIONS



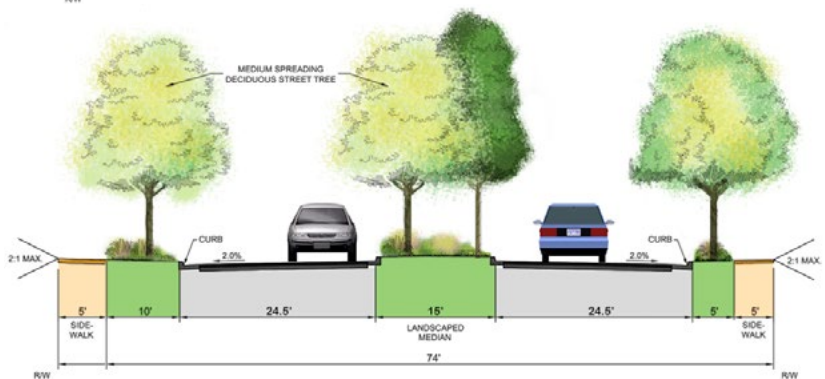
C STREET A - 85' R.O.W.



STREET A - 74' R.O.W. **D**



E STETSON AVENUE - 63' R.O.W.



STREET E - 74' R.O.W. **F**

EXHIBIT 4-3. STETSON AVENUE, STREETS A AND E STREET SECTIONS

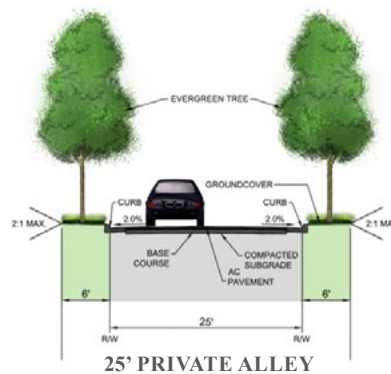
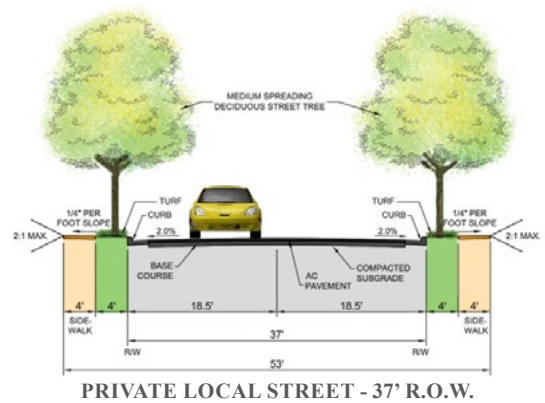
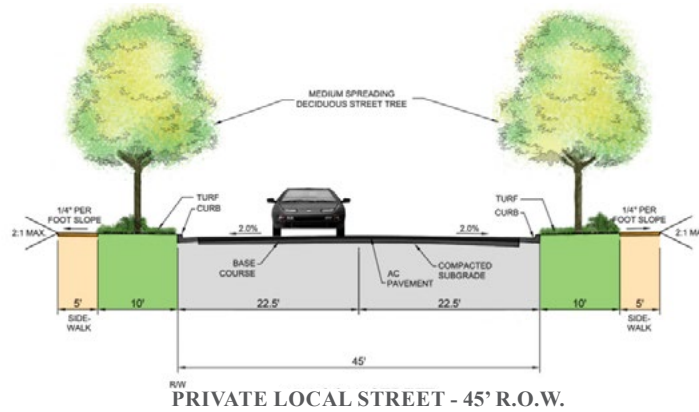
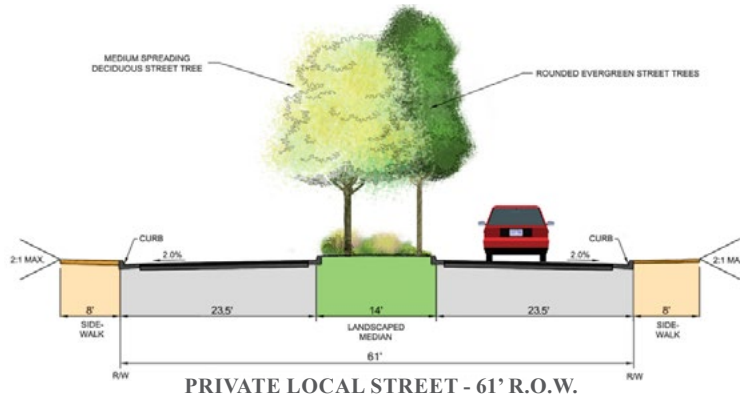
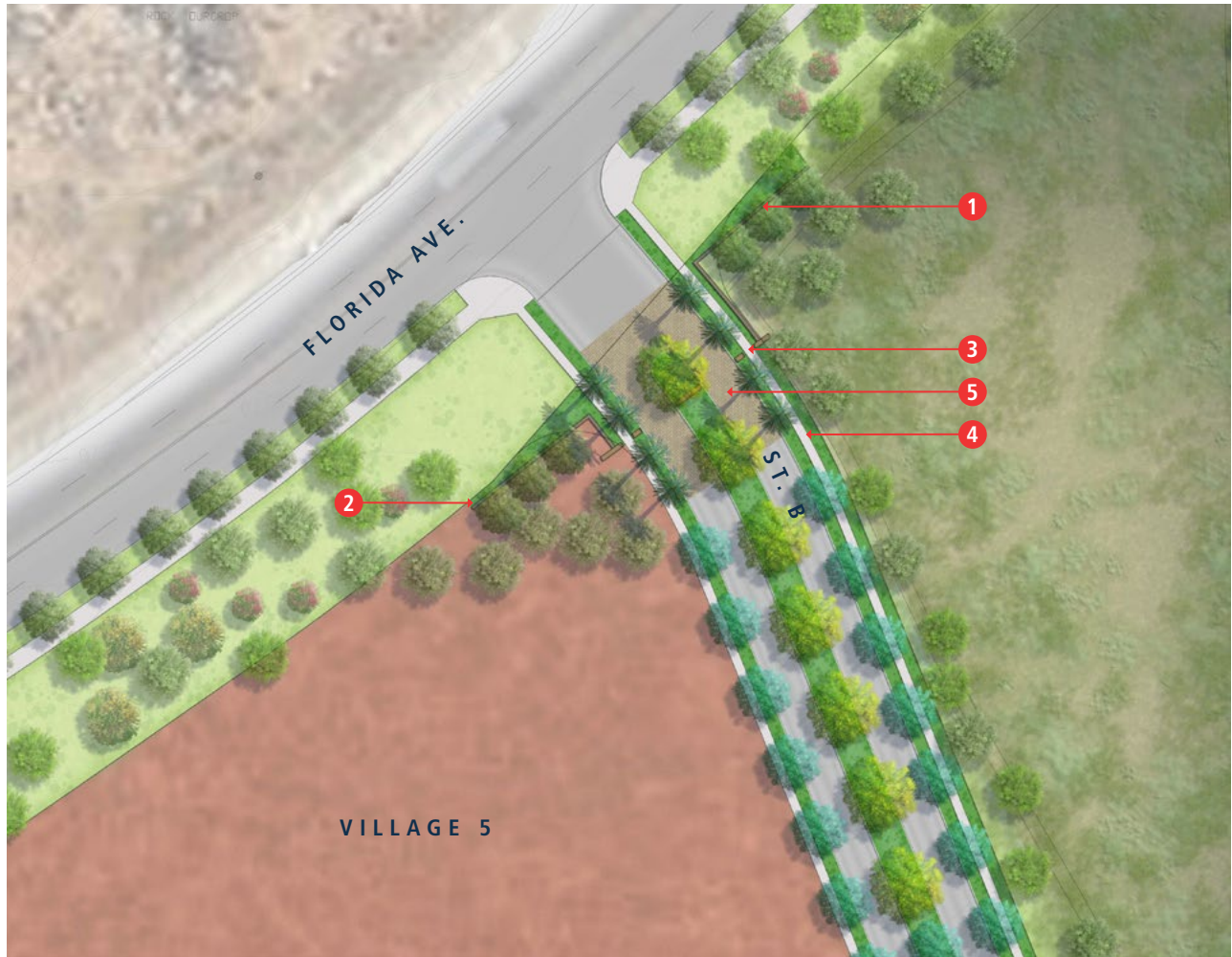


EXHIBIT 4-4. PRIVATE LOCAL STREETS AND ALLEY STREET SECTIONS



PROJECT ENTRY ENLARGEMENT

LEGEND

- 1 Steel Sign with Pin Mounted Letters
- 2 Stone Wall
- 3 Entry Portal
- 4 Sidewalk
- 5 Enhanced Vehicular Paving

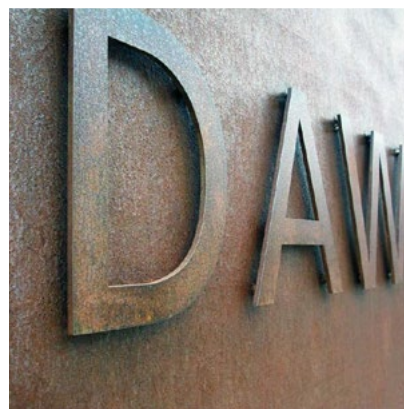
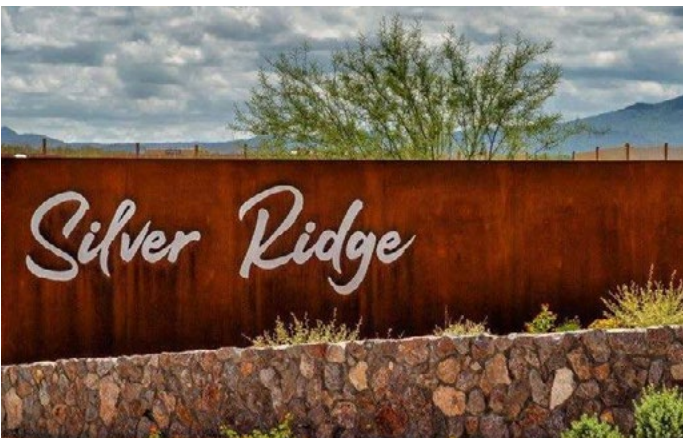
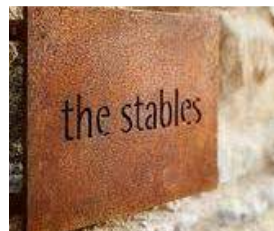
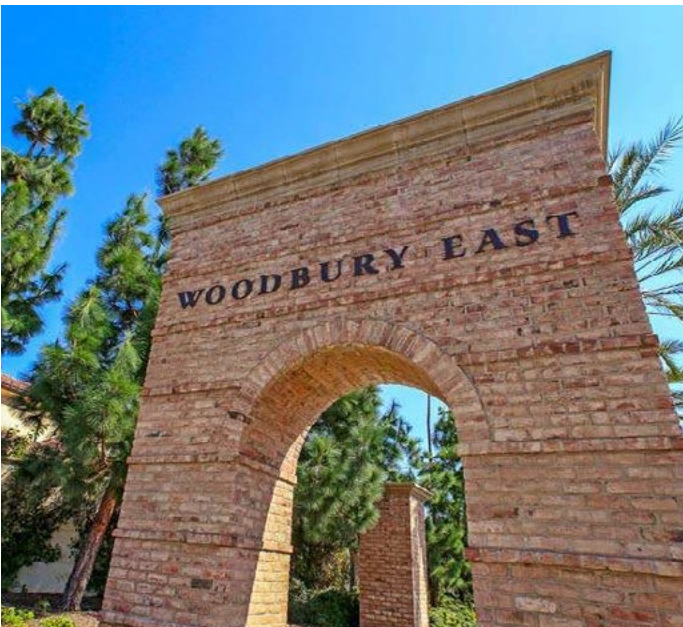
KEYMAP



EXHIBIT 4-5. PROJECT ENTRY



PROJECT ENTRY SECTION



that Emerald Acres is a special place to live; and to provide an entry statement emphasizing the hillside character and unique nature of the site. The hierarchy of entry monumentation consists of one primary entry monument, seven (7) neighborhood entry treatments, and two (2) special treatment intersections. The entry settings are comprised of a blending of natural hardscape materials, signage and landscaping which introduce the theme of the community.

4.2.3.1 PROJECT ENTRY

The Project Entry is located at the northerly terminus of Street E at the intersection with Florida Avenue. As shown in Exhibit 4-5, Project Entry, the design of this primary entry monument incorporates both planting and hardscape features including colors, forms, and shapes consistent with the overall design theme.

The Project Entry demonstrates the community's landscape theme, establishes the character of the community landscape treatments, and provides a prominent reminder of the quality and distinctiveness of this community.

Specifically, the Project Entry Monumentation consists of the following:

- Formal planting of palm trees along the entry frontage from Florida Avenue
- 5-foot wide sidewalk on both sides of the entryway with accompanying 10-foot wide parkway with flowering ground cover and low shrub massing and flowering/medium spreading deciduous trees
- 24.5-foot wide lanes in each direction with enhanced accented paving and concrete banding
- 15-foot wide landscaped median that includes small evergreen trees
- A roundabout with flowering ground cover and low shrub massing
- Large evergreen entry trees that surround outer rim of roundabout
- Accent shrubs intermixed with the groundcover to create varying shrub masses of interesting textures and colors
- 6-foot high View Fence that separates the entry road from commercial area to the west and open space area to the east

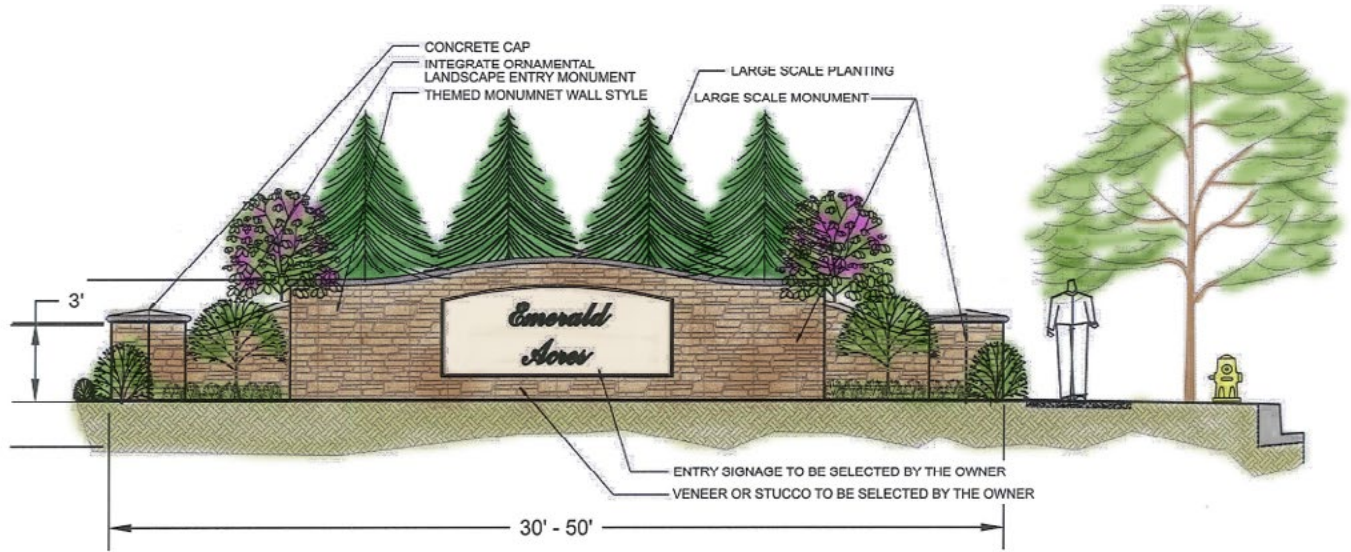
4.2.3.2 PRIMARY AND NEIGHBORHOOD ENTRY

Primary and Neighborhood Entries shall be provided as illustrated in Exhibit 4-6, Primary and Neighborhood Entry. Two (2) Primary and nine (9) Neighborhood entries convey the overall community identity by repetition of community entry plant materials. The Primary and Neighborhood entries also feature the following components:

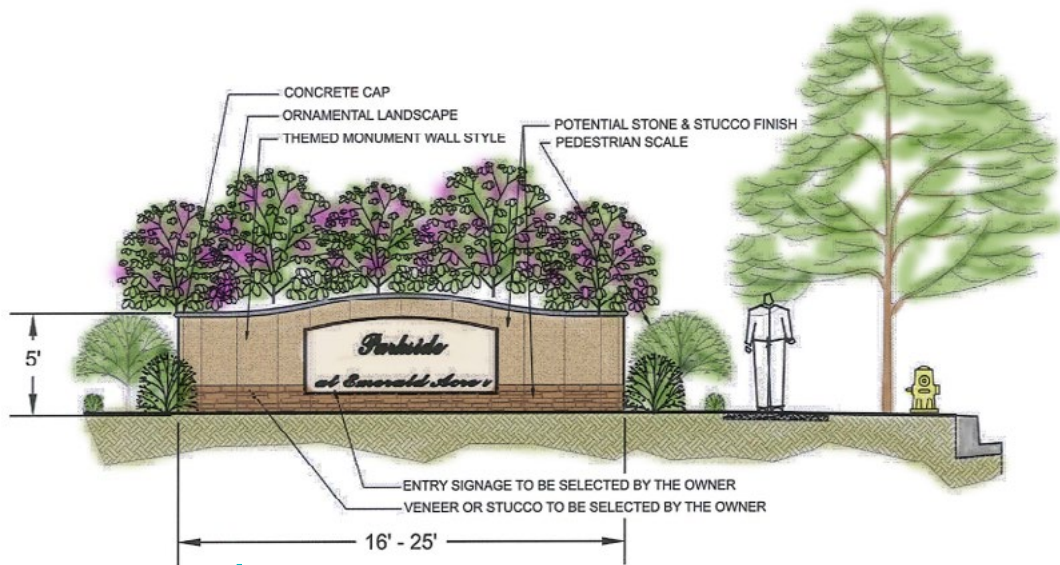
- Enhanced accent paving bordered by complimentary concrete banding;
- Formal planting of standard/rounded canopy evergreen neighborhood trees, large evergreen specimen trees, flowering/medium spreading deciduous trees, and flowering ground cover;

4.2.3.3 SPECIAL TREATMENT INTERSECTIONS

Special treatment intersections provide another means of reinforcing the community theme as shown in Exhibit 4-7, Roundabout Detail. These roundabout intersections, which occur along Street A at the boundary between Villages 1 and 2, serve as traffic control features as well as a way to signal a change in density and neighborhood feel. Large specimen evergreen entry trees envelop the enhanced accent paving



*** PRIMARY ENTRY**



*** NEIGHBORHOOD ENTRY**

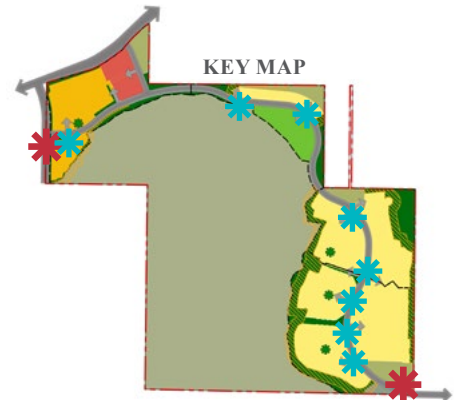
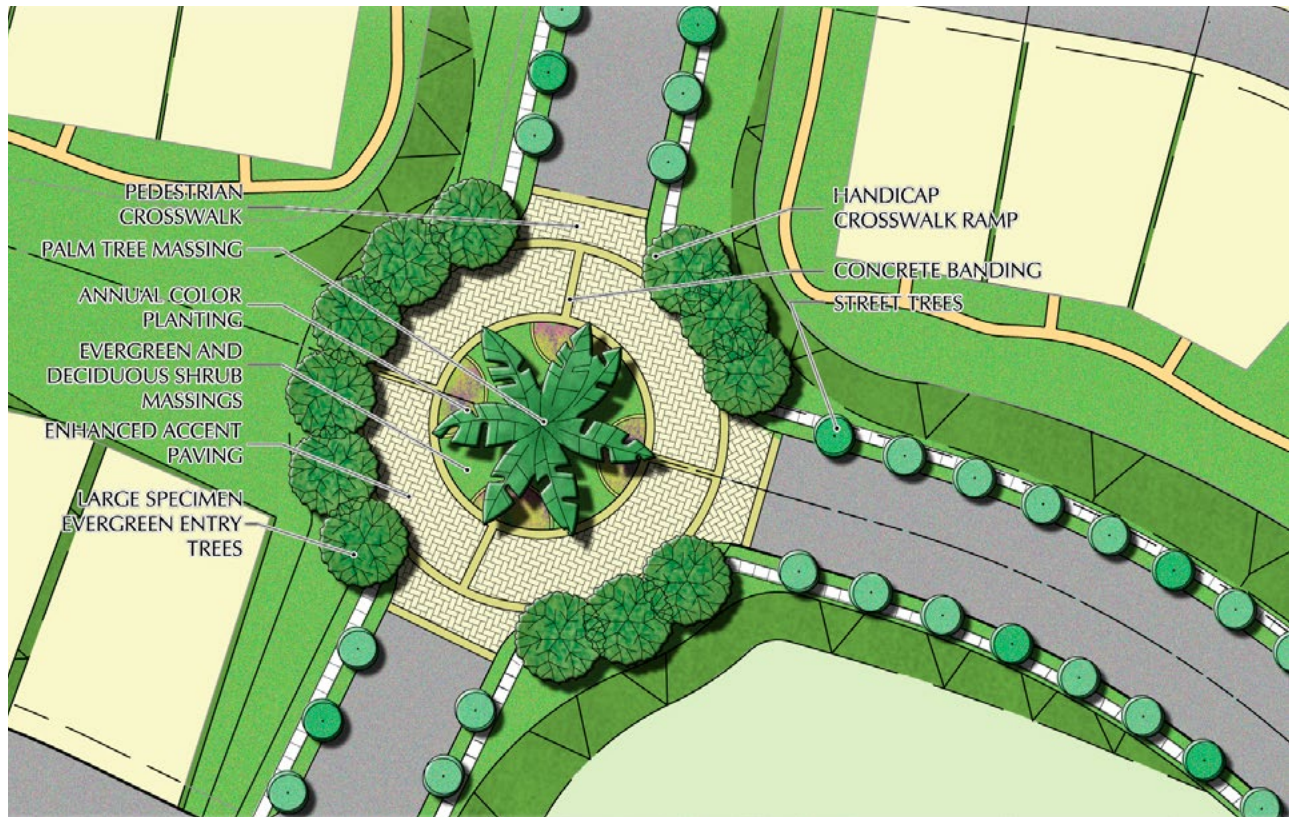


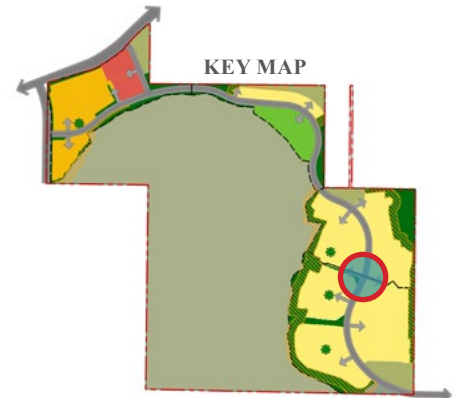
EXHIBIT 4-6. PRIMARY AND NEIGHBORHOOD ENTRIES



PLAN VIEW



PERSPECTIVE



KEY MAP

EXHIBIT 4-7. ROUNDABOUT DETAIL

within the roundabout, with a planting of palm trees complimented with evergreen and deciduous shrubs providing a focal point in the interior of the intersections.

4.2.4 RECREATIONAL AMENITIES

Critical to the image of Emerald Acres is the quality of the community spaces and their proper integration into the neighborhood fabric. Recreational and open space amenities increase opportunities for Emerald Acres residents to enhance their quality of life through outdoor activities. The park and recreation design program has several major recreational elements including a multi-use park, village recreation areas, trails, greenbelts and natural open space facilities.

Recreational parks and passive parks are fundamental to the success of any community. Emerald Acres features both active and passive parks as a means of providing outdoor recreation. Trails are planned throughout the community to connect the residential areas to the 6.6-acre park located within Planning Area 9.

4.2.4.1 ACTIVE NEIGHBORHOOD PARK

As shown in Exhibit 4-9, Active Neighborhood Park, a 6.6-acre public park area is located in Planning Area 9, adjacent to Villages 1 and 2. Core amenities may include a basketball court, baseball field, soccer field, picnic grove, tot lot, restroom facilities, and on-site parking. Other design features are discretionary and may include community gathering areas, shade structures, passive recreation lawns, and barbecue area. Access to the public park is provided via Street A and adjacent greenbelts.

4.2.4.2 VILLAGE RECREATION AREAS

Village Recreation Areas are located within Villages 1, 2 and 4, and provide recreation or open space needs to residents within Emerald Acres distinctive from regional, community or neighborhood parks. The primary purpose of these recreational areas is to provide a localized recreational area for residents in Villages 1 and 2. As shown in Exhibits 4-10 through 4-13 Village Recreation Area Details, these small parks may include elements such as tot lots, community gardens, dog parks, gazebos, benches, shade arbor seating areas, game tables, bocce ball/shuffleboard courts, trail connections, picnic groves, and open turf areas.

4.2.4.3 DETENTION BASINS

Stormwater management within Emerald Acres utilizes a system of debris basins, drainage channels and water quality basins, to control stormwater flows and eliminate historic flooding of adjacent properties. Some of the detention basins may contain landscaping designed to provide opportunities for passive recreation. Four detention basins are located within Planning Areas 3, 7, 11, and 14. In total, there are 11.0 acres of detention basins within the community.

4.2.4.4 EQUESTRIAN TRAIL

An Equestrian Trail is located within the southeastern portion of Village 1 (between Village 1 and PA 6) and connects to the existing trail network south of the Project site. The trailhead point-of-entry is located between PA 1B and PA 4 in Village 1, along Street A, and the trail continues through PA 2 south of Village 1 before meandering to the natural open space within Planning Area 16. The equestrian trail is open to the public and will be maintained by a landscape maintenance district or functional equivalent.



LEGEND

- ① Active Neighborhood Park
- ② Village 1 Park A
- ③ Village 1 Park B
- ④ Village 2 Park C
- ⑤ Village 4 Park E
- ⑥ Primary Entry

EXHIBIT 4-8. OVERALL LANDSCAPE CONCEPT



LEGEND

- 1** Public Parking Lot (6 Stalls Per Acre Minimum)
 - With ADA Access
- 2** U11 Soccer Field
- 3** Baseball Field 250'
 - Standard Backstop Per District Detail
 - ADA Accessible To The Bleacher
 - Skinned Infield
- 4** Public Restroom Building, ADA Accessible
- 5** (2) Half-Court Basketball Courts
- 6** Play Areas- 2 To 5
- 7** Play Areas- 5 To 12
- 8** Shade Structure With Picnic Tables/BBQ/Trash Receptacle
- 9** Warmup Practice Open Turf Area
- 10** Picnic Area
- 11** 8' Wide Perimeter Walkway With Benches And Trash Receptacles



EXHIBIT 4-9. ACTIVE NEIGHBORHOOD PARK



LEGEND

- 1 Sign Wall With Specimen Tree
- 2 Social Space With Lounge Chairs And Dining
- 3 Children's Play- 2 To 5
- 4 Children's Play- 5 To 12
- 5 Play Lawn
- 6 Screening Trees
- 7 Street Trees

KEYMAP

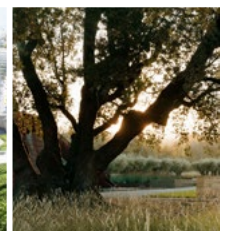
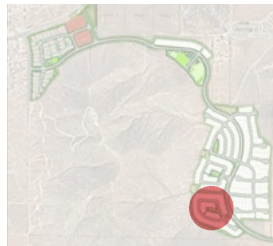


EXHIBIT 4-10. VILLAGE 1-PARK A RECREATION AREA DETAILS



LEGEND

- 1 Shade Trellis With Laser-Cut Decor Screen Under Date Palm
- 2 Bocce Ball Courts With Seatwall
- 3 Specimen Tree In Decor Gravel Bed
- 4 Pedestrian Walk/Staircase
- 5 Palm Tree Grove
- 6 Event Lawn
- 7 Tot Lot
- 8 Screening Trees

KEYMAP

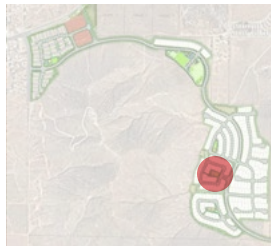


EXHIBIT 4-11. VILLAGE 1-PARK B RECREATION AREA DETAILS



LEGEND

- 1 Shade Structure With Vine Planting
- 2 Specimen Tree
- 3 Flowering Tree Grove With Benches
- 4 Event/Play Lawn
- 5 Children's Play With Seatwall
- 6 Street Trees



KEYMAP

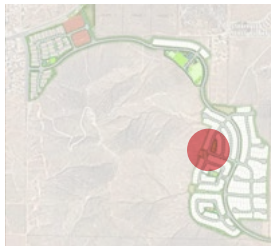


EXHIBIT 4-12. VILLAGE 2-PARK C RECREATION AREA DETAILS



LEGEND

- 1 Shade Structure With Picnic Tables And Barbecue
- 2 Half-Court Basketball Court
- 3 Specimen Tree In Decor Gravel Pocket
- 4 Event /Play Awn
- 5 Children's Play
- 6 Screening Trees With Benches
- 7 Street Trees



KEYMAP

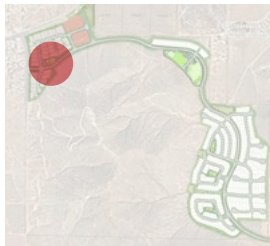


EXHIBIT 4-13. VILLAGE 4-PARK E RECREATION AREA DETAILS

4.2.5 COMMERCIAL PLANNING AREA

The Emerald Acres community is served by a 5-acre commercial planning area within Planning Area 13 in Village 4. Planning Area 13 is intended provide residents and non-residents alike with a range of convenient retail options. The commercial area is accessible via Street E, Street A, a shared access street in between PA 12A and PA 13, and the trail located along Street A.

4.2.6 LAND USE TRANSITIONS

Within Emerald Acres Specific Plan, transitions between land uses will occur at the boundaries separating the various residential, recreational, commercial and open space areas. The following sections and exhibits describe and illustrate the transitions which will occur between various land uses within the community.

4.2.6.1 STREET A/RESIDENTIAL INTERFACE

As previously shown on Exhibit 4-1, Conceptual Landscape Key Map, all Villages throughout the Emerald Acres community share an interface with Street A that require special treatments and specific edge conditions to provide for seamless land use transitions from residence to road. As shown on Exhibit 4-14, Street A/Residential Edge Condition, residential boundaries that are higher in elevation than Street A include a View Fences at the edge of the residential use with slope tree plantings and shrub/groundcover along the manufactured slopes to improve the aesthetics from both roadway and residential viewpoints. Residential boundaries that are lower in elevation than the road include slope tree planting along manufactured slopes and Solid Wall barriers.

4.2.6.2 RESIDENTIAL/MANUFACTURED SLOPE/DETENTION BASIN INTERFACE

Detention basins are provided to direct, store, and filter storm water throughout the Emerald Acres community. Special treatments are required along detention basin boundaries that are adjacent to residences (see Exhibit 4-1 for locations of these interfaces). As shown on Exhibit 4-15, Residential/Manufactured Slope/Detention Basin Edge Condition, in all cases, residences are located at higher elevations than the detention basins. Transition areas between residences and detention basins include the use of View Fences, slope tree planting and shrub/groundcover along a manufactured slope. Multi-use trails consisting of decomposed granite surfaces are permitted within the Residential/Manufactured Slope/Detention Basin Interface areas.

4.2.6.3 RESIDENTIAL/OFF-SITE INTERFACE

Special treatment is provided to the boundaries of residential Villages that are located directly adjacent to the Emerald Acres Specific Plan boundary (see Exhibit 4-1 for locations of these interfaces). As shown on Exhibit 4-16, Residential/Off-Site Edge Condition, edge conditions in these portions of the community include a View Fence with slope tree planting and shrub/groundcover on manufactured slopes to create natural and aesthetically pleasing buffers between the residences of Emerald Acres and neighboring communities.

4.2.6.4 RESIDENTIAL/OPEN SPACE EDGE CONDITION WITH FIRE FUEL MODIFICATION ZONE

The Emerald Acres community is located within a designated “Hazardous High Fire Area,” where natural open space areas on- and off-site may increase the fire risk for future residences. As previously shown on Exhibit 4-1, Conceptual Landscape Key Map, Fire Fuel Modifications Zones (FMZ) are provided throughout the Emerald Acres community along all residential boundaries adjacent to the natural open space within Planning Area 16. As is shown on Exhibit 4-17, Residential/Open Space Edge Condition with

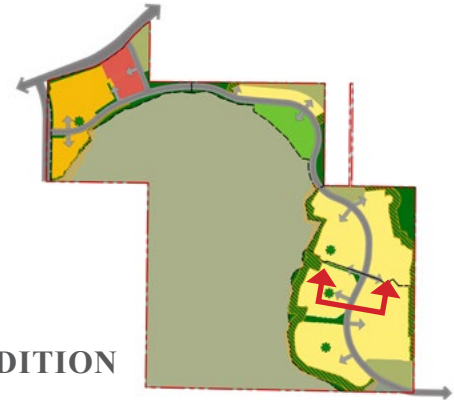
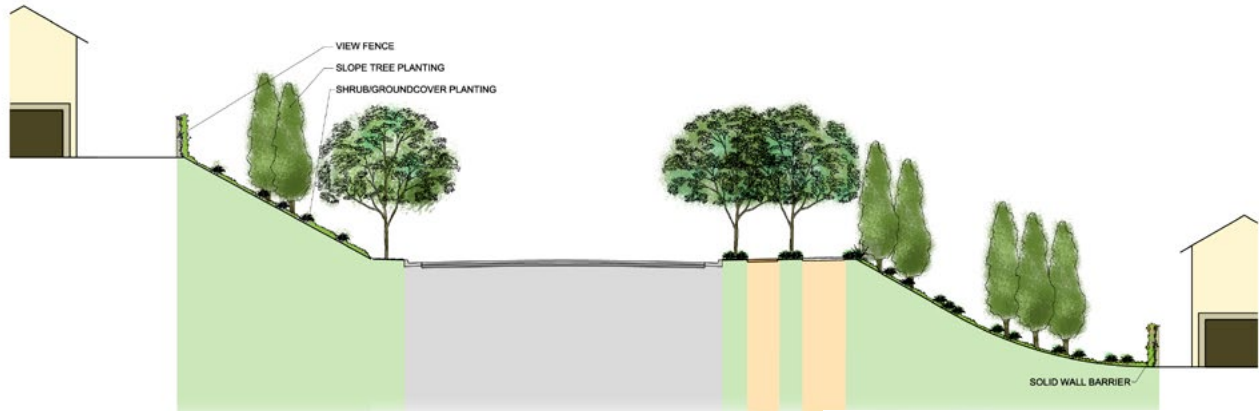


EXHIBIT 4-14. STREET A/RESIDENTIAL EDGE CONDITION

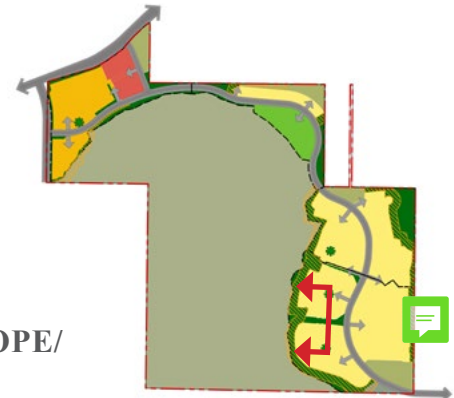
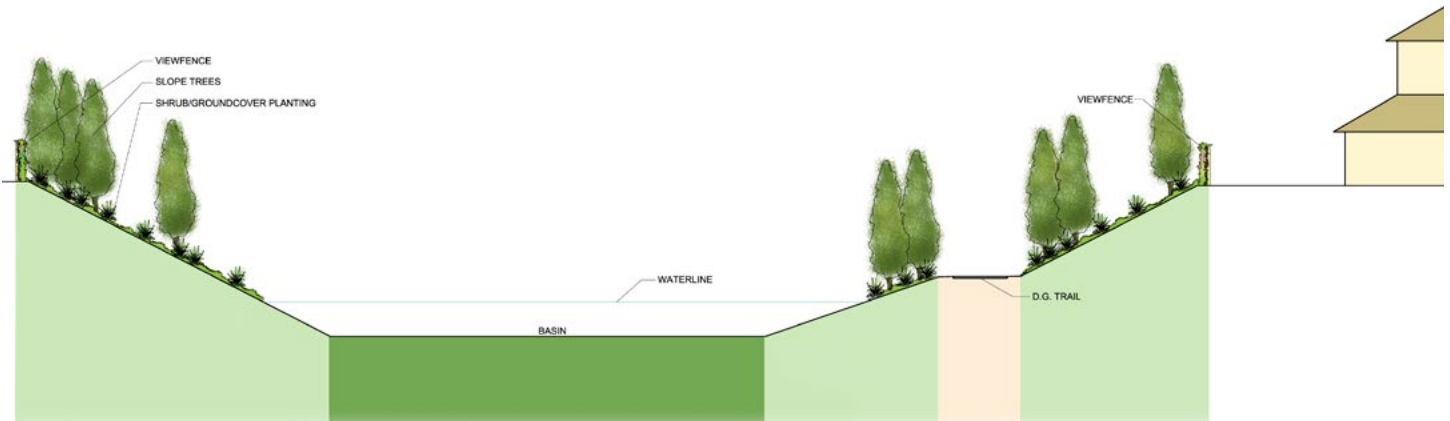


EXHIBIT 4-15. RESIDENTIAL/MANUFACTURED SLOPE/
DETENTION BASIN EDGE CONDITION

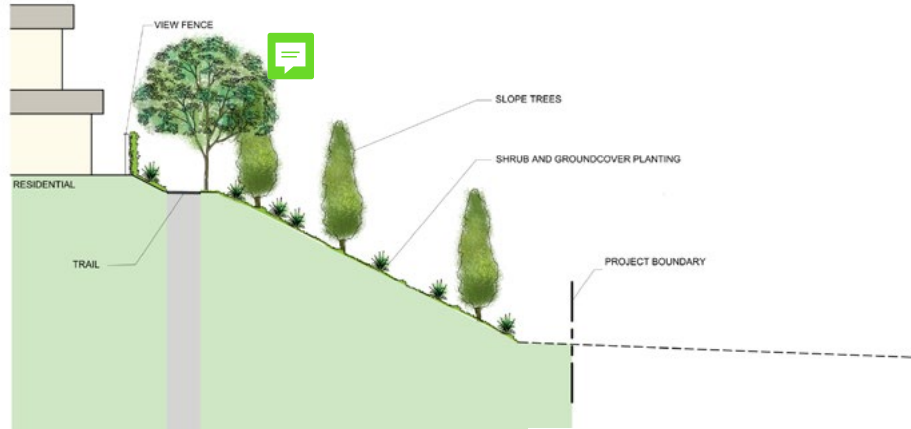


EXHIBIT 4-16. RESIDENTIAL/OFFSITE EDGE CONDITION

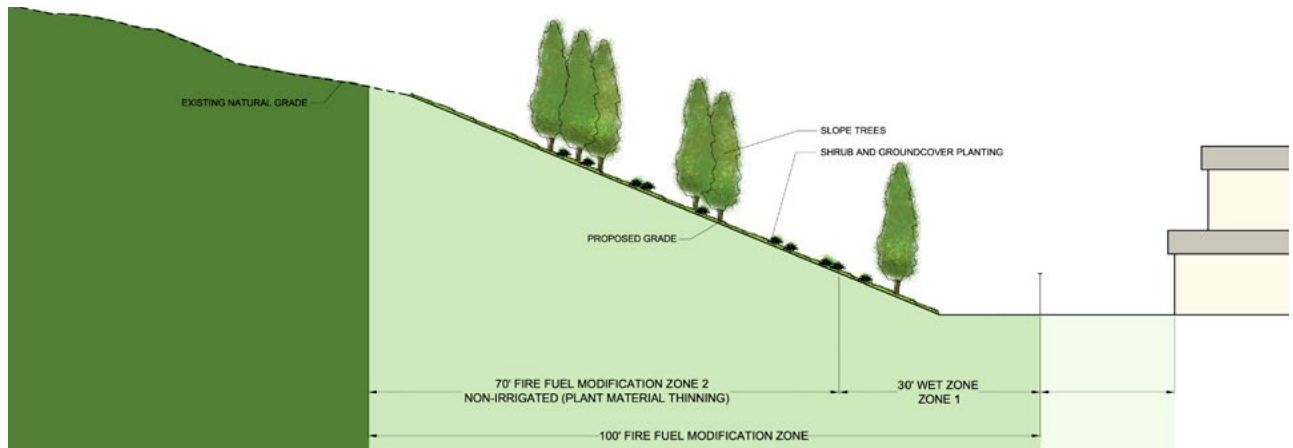


EXHIBIT 4-17. RESIDENTIAL/OPEN SPACE EDGE CONDITION WITH 100' FIRE FUEL MODIFICATION ZONE

100' Fire Fuel Modification Zone, these boundaries establish an FMZ that consists of a minimum of 100'-wide buffer area, measured from the structure into the natural open space area, in order to reduce potential impacts from fires in the nearby natural open space.

The Fire Fuel Modification Zone is divided into 2 zones as follows:

- Zone 1: occurs within the first 30' from the structure (at the toe of the slope) and consists of irrigated shrub and groundcover planting
- Zone 2: occurs within the 70' immediately beyond Zone 1 (within the area of the graded slope) and consists of non-irrigated slope trees, shrubs, and groundcover planting

For Fire Fuel Modification Zones located on individual lots, the private lot owner will be responsible for brush management. In order to mitigate local fire hazards, selective plant removal will be undertaken to improve the safety of the residents within the Emerald Acres community. Where a roadway occurs within a FMZ, the width of the FMZ is measured to the edge of the pavement nearest to the residential land uses and the roadway width is incorporated into the 30' irrigated "Zone 1".

Fire Fuel Modification Zones within the Emerald Acres community may include reductions in the width of the irrigated "Zone 1" based on the plotting of individual homes, where necessary. Where these reductions occur, the following fire abatement measures are required:

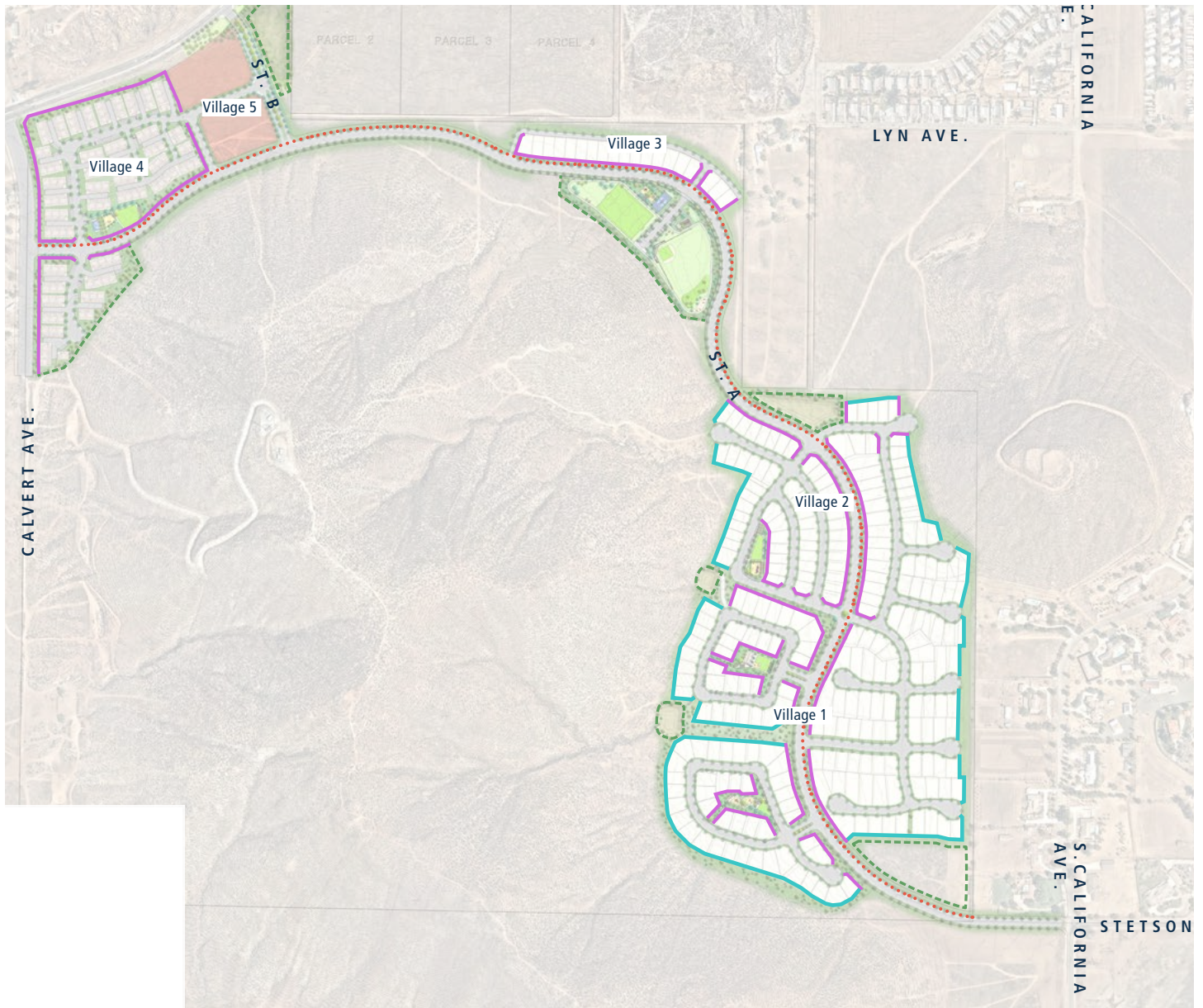
- A non-combustible radiant heat wall shall be provided between the structure and the adjacent open space;
- The habitable structure within the reduced areas of Zone 1 shall include fire-rated construction materials;
- Reductions in the width of the non-irrigated "Zone 2" Fire Fuel Modification Zone are permitted where necessary based on the plotting of individual homes. Where these reductions to the non-irrigated "Zone 2" occur, the following fire abatement measure is required; and
- A non-combustible radiant heat wall shall be provided between the structure and the adjacent open space;

4.2.7 COMMUNITY WALLS AND FENCING PLAN

Walls and fencing will be used throughout the community to complement the overall design theme, establish community identity, provide protection from roadway and other noise, and allow privacy and security in residential areas. Four types of walls and fencing are proposed to be used within this Emerald Acres Specific Plan, as illustrated in Exhibit 4-19, Conceptual Wall and Fence Details. In addition, production fencing between lots shall be provided.

4.2.7.1 COMBINATION SOLID WALL/VIEW FENCE

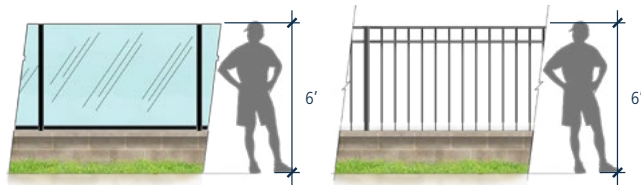
These fence types will be used at parks and open space areas where some security and privacy is necessary, but also where the view is desirable to adjacent neighborhoods or roads. As shown in Exhibit 4-19, combination Solid and View Fencing has a total height of 6' and consists of a 4' tall glass insert above a 2' tall masonry stem wall. Pilasters may be covered with brick, ceramic tile, stone, or stucco, and may have decorative caps from the same material options. Pilasters may be provided at regular intervals, including changes in elevation or direction.



LEGEND

-  Combination Solid Wall/View Fence
-  Solid Wall
-  3-Rail Split Fence
-  View Fence
-  Sideyard Wood/Vinyl Fence
Note: Used for side yard conditions within residential areas.

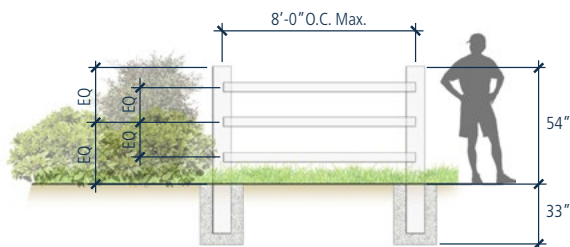
EXHIBIT 4-18. CONCEPTUAL WALL AND FENCE PLAN



COMBINATION SOLID WALL/VIEW FENCE
SCALE: N.T.S.



SOLID WALL
SCALE: N.T.S.



3-RAIL SPLIT FENCE
SCALE: N.T.S.



VIEW FENCE
SCALE: N.T.S.



SIDEYARD WOOD/VINYL FENCE
SCALE: N.T.S.

Note: Used for side yard conditions within residential areas.

EXHIBIT 4-19. CONCEPTUAL WALL AND FENCE DETAILS

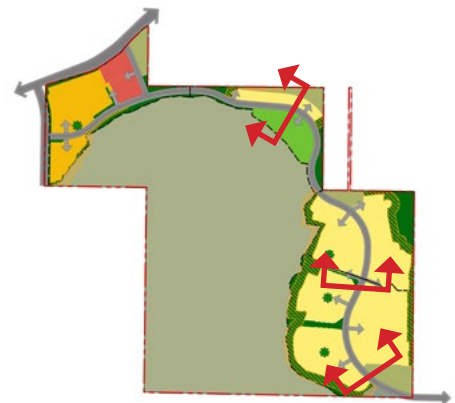
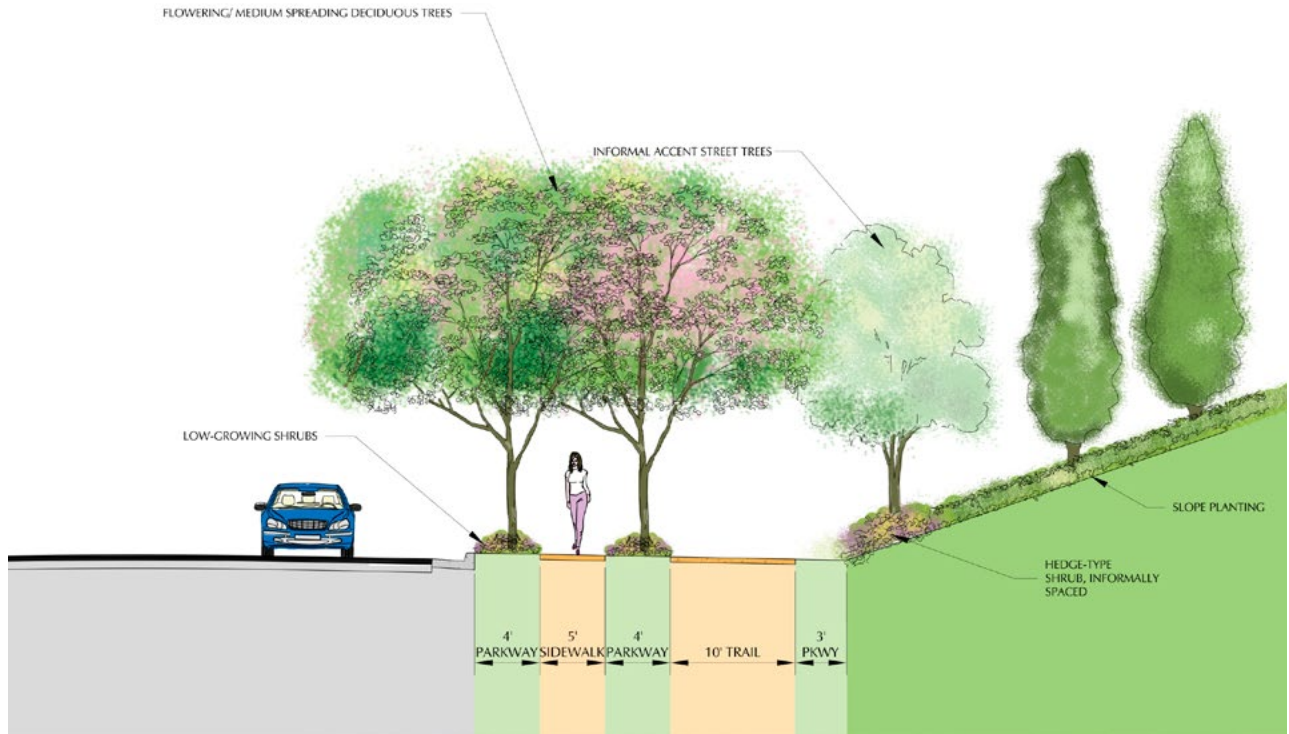


EXHIBIT 4-20. WALKWAY DETAIL

4.2.7.2 VIEW FENCE

These fence types will be used at open space areas that abut residential spaces where the view is desirable to adjacent neighborhoods or roads. As shown in Exhibit 4-19, combination View Fencing has a total height of 6' and consists of a tubular steel pickets. Plain concrete block walls, wood, stucco-covered or other like-material walls are not permitted along reverse frontage areas of the View fences.

4.2.7.3 SOLID WALL

This wall will be used within the Specific Plan for a variety of purposes, where privacy or security is desired, as well as in locations where screening is necessary. As shown in Exhibit 4-18, installation of solid walls will typically occur where residential neighborhood abut public streets (i.e. between Village 1 and Street A). The solid wall is 6 feet in height and constructed with 8" x 6" x 16" masonry bricks, see Exhibit 4-19, Conceptual Wall and Fence Details. Pilasters may be covered with brick, ceramic tile, stone, or stucco, and have decorative caps consistent with the Project entry monumentation.

Pilasters may be provided at regular intervals, including changes in elevation or direction. Plain concrete block walls, wood, stucco-covered or other like-material walls are not permitted along reverse frontage areas (lots which include rear yards abutting a major street). The use of ivy or other vegetative material is highly encouraged to reduce the likelihood of graffiti on community walls. Wall height may not exceed 6 feet, unless necessary for noise attenuation or other special circumstances.

4.2.7.4 WOOD/VINYL FENCING

These fence types will only be used for side yard conditions within residential areas where some security and privacy between different homes is necessary. As shown in Exhibit 4-19, combination Wood/Vinyl Fencing has a total height of 6' and consists of wooden or vinyl planks. Planting of shrubs is encouraged along all wood/vinyl fence frontages.

4.2.8 MULTI-USE TRAIL

The landscaping concept for the Emerald Acres trail network emphasizes a formal pedestrian environment that is easily accessible to all residents of the community. The Multi-Use Trail consists of a 10' wide decomposed granite surface and runs along the northbound side of Street A from Stetson Avenue to Calvert Avenue. The northbound streetscape of Street A provides 4' of landscaped parkway between the 5' sidewalk and the paved road and 4' of landscaped parkway between the sidewalk and the Multi-Use Trail. Although the Multi-Use Trail does not occur along the southbound side of Street A, a 5' sidewalk is provided from Stetson Avenue to the boundary of Village 2 and 3. Between Village 3 and Calvert Avenue, the Street A streetscape narrows and a sidewalk is provided only on the northbound side of the roadway. Accent trees are used extensively within the adjacent parkways to provide an attractive pedestrian experience and offer privacy and security for adjacent homeowners (see Exhibit 4-20, Walkway Detail).

4.2.9 MANUFACTURED SLOPES

As part of community development, gentle 4:1 manufactured slopes are located adjacent to some roadways and/or within open space areas that contain Fire Fuel Modification Zones.

Where manufactured slopes occur within residential areas or at the interface of residences and Street A, the following criteria will be followed (see also Exhibit 4-14, Street A/Residential Edge Condition):

- Slopes may be constructed to undulate both horizontally and vertically, varying between ratios of 4:1 and 2:1. No slope may exceed a ratio of 2:1.

- Landscaping shall be installed as quickly as possible upon completion of slope construction to reduce the potential for erosion.
- The use of plant species with the ability to stabilize slopes and prevent erosion, including species which can be quickly planted, may be encouraged on all manufactured slopes.
- View fences shall be placed at the top of the slope to separate residences situated at higher elevations than Street A from manufactured slopes.
- Solid Wall barriers will be placed at the toe of the slope to separate residences situated at lower elevations than Street A from manufactured slopes.

Where manufactured slopes occur within or adjacent to Open Space – Conservation Habitat areas (i.e. Fire Fuel Modification Zones), the following criteria will be followed (see also Exhibit 4-17, Residential/Open Space Edge Condition with 100' Fire Fuel Modification Zone):

- Slopes may be constructed to undulate both horizontally and vertically, varying between ratios of 4:1 and 2:1. No slope may exceed a ratio of 2:1.
- Landscaping shall be installed as quickly as possible upon completion of slope construction to reduce the potential for erosion.
- The use of plant species with the ability to stabilize slopes and prevent erosion, including species which can be quickly planted, may be encouraged on all manufactured slopes.
- Slope irrigation and planting should be divided into 3 zones as is illustrated on Exhibit 4-17.

4.2.10 SIGNAGE

The community has a mixture of residential, commercial, and recreational uses. The type of signage discussed below is desirable on a community-wide basis.

4.2.10.1 GENERAL INTENT

In general, signage should be consistent with the community's image as a high quality, well balanced, residentially-oriented development with extensive recreational opportunities. Signage may be attractive and subdued in keeping with the character of the community.

- Signs shall be limited to community or store identification and direction identification only.
- Signs which advertise products and other merchandise are prohibited.
- No signs shall be painted directly on any residential building elevation, except street numbers on houses.
- All ground mounted signs shall be well-integrated into site landscaping.
- Temporary "For Sale" signs shall be permitted.
- More specific design standards, including dimension and location criteria, shall be formulated as part of the implementation of the community through mechanisms such as CC&R's.

4.2.10.2 COMMERCIAL SIGNAGE

Signage within the Commercial Planning Area shall harmoniously blend with the architectural theme. Artistic flexibility is allowed; however, signage shall maintain continuity and be of appropriate scale to the

community as a whole. Specifically, signs within commercial portion of the Emerald Acres community shall conform to the following guidelines:

- Sign types shall be limited to project identification, company and building identification, direction, and temporary “for sale and lease” signs.
- Signs should be designed to be complementary with the building they identify.
- Signs shall be compatible with the building in terms of color, material, and placement yet stand out in a way that will enable easy recognition of the sign.
- Building and company identification signs shall be low to the ground or attached to building facades. No pylon or pole-mounted signs are permitted.
- All ground-mounted signs shall be well integrated into site landscaping. The exposed backs and sides of signs shall be architecturally treated to blend with the exterior character of the buildings.
- Commercial signs shall be consistent with all applicable County ordinances and/or regulations related to commercial signage.

4.2.10.3 PROHIBITED SIGNS

- Revolving, rotating, or moving signs shall be prohibited, except that pennants, banners or flags used in conjunction with subdivision sales offices and tract entry points may be permitted during the initial sales period to the extent permitted by the County Ordinance.
- Signs with flashing or blinking lights shall be prohibited.
- Roof signs shall be prohibited.
- Signs may not project above any roofline.

4.3. ARCHITECTURAL DESIGN GUIDELINES

Architectural Design Guidelines define the design concept, physical character, and theme of Emerald Acres. The Architectural Design Guidelines are intended to provide a basis for decisions on how the structural environment is to be built. A high-quality living environment is defined by the guidelines for site planning, architectural theme and details, building mass and scale, materials and color, and articulations.

The architectural goal of Emerald Acres is to provide residential homes and a commercial area that exhibit high quality design, offer a variety of housing opportunities, and are sensitive to their historical context. The architecture for each home is envisioned to be created through thoughtful analysis and consideration of the development’s theme, as well as the constraints, opportunities, and characteristics of each neighborhood. Architectural, residential, and commercial construction standards are intended to conform to and address site conditions/characteristics such as fire hazards and drought tolerant landscaping.

A variety of architectural styles is essential to creating a high-quality community. Emerald Acres features four unique architectural styles that adhere to the overall Community Theme of early California. The architectural styles for residential units within the Emerald Acres community include Craftsman, Bungalow, Spanish and Monterey. These compatible architectural styles provide a wide range of architectural variation, which appeals to a variety of potential homeowners and create visually interesting street scenes. Guidelines and criteria are provided within Exhibit 4-21 through Exhibit 4-28 and address the peculiarities and thematic details of each architectural style. Additionally, conceptual images of the product type and lot sizes offered throughout Emerald Acres are included within Exhibit 4-29 through Exhibit 4-34.

Developers, builders, engineers, architects, landscape architects and other design professionals should use the guidelines to maintain design continuity, create an identifiable image, and develop a cohesive community. While this document provides illustrations of the chosen architectural styles, these are only intended to impart the style's characteristic elements and are not meant to depict the exact residences to be constructed. Rather, they are intended to foster flexibility and provoke design ideas that ultimately produce a community of quality and cohesiveness.

The following descriptions and referenced graphics provide an overview of the general architectural styles, details and typical product elevations desired for Emerald Acres. Again, it should be emphasized that individual character and interpretation are encouraged and it is not the intent that all of the represented design components be incorporated into each residence or neighborhood. These examples are conceptual in nature and are meant to provide a menu of options that suggest the selected architectural style. Finalized floor plans and elevations will be determined at a later stage of development, although illustrative building footprints for the various lot sizes are shown in this document.

4.3.1 RESIDENTIAL DESIGN GUIDELINES – SINGLE-FAMILY DETACHED (VILLAGES 1, 2, AND 3)

Single-Family Detached homes on fee simple lots are provided throughout Villages 1, 2 and 3 and shall reflect a variety of themes and innovative styles contributing to the integrity and character of the Emerald Acres community. Neighborhoods are built over time, and each home possesses its own unique identity and features. In order to evoke this sense within Emerald Acres, each residential neighborhood shall provide diversity in design through considerate attention to architectural character and floor plan livability. To promote this goal, the following standards shall be applied:

In all single-family detached neighborhoods, provide:

- A minimum of three (3) plans for every Village; and,
- A minimum of four (4) architectural styles for Villages 1-3

4.3.1.1 SINGLE-FAMILY RESIDENTIAL ARCHITECTURAL ELEMENTS

The guidelines in this section address important residential architectural elements, such as roofs, visible edges, garage placement, and outdoor living areas. Together, these key elements can facilitate the creation of diverse and varied streetscapes while also maintaining a cohesive sense of place. This section contains general requirements that apply to all single-family residential neighborhoods within Emerald Acres.

The successful development of pedestrian-friendly and family-oriented neighborhoods within Emerald Acres can be achieved by implementing the following goals:

- Provide innovative plans by avoiding repetitious designs, massing, roof forms, colors, elevations and footprints.
- Provide architectural styles that complement building form.
- Incorporate rear and side architectural details along visible edges.
- Encourage the integration of indoor and outdoor living spaces.
- Design the composition of building mass with integrity and character.
- Provide interesting, pedestrian-scale architectural elements along street frontage.
- Minimize the visual impact of the garage.

Visible Edges

In order to ensure that residential development within Emerald Acres is visually pleasing, all visible edges of homes within the community are subject to a certain degree of articulation.

Roofs: Rows of homes seen from a distance or along arterial roads are perceived in relation to their contrast against the skyline or background. Therefore, the primary visual impact from this viewpoint is the shape of the building and roofline. Providing a variety of hip, gable, and shed roof forms, as well as providing a mixture of one- and two-story homes and one- and two-story elements, can help avoid a monotonous edge condition.

Corner Lot Homes: Homes located on corner lots shall provide the same level of articulation on the publicly visible side elevation as on the front elevation. Corner homes are encouraged to incorporate the following design elements on the side elevation:

- Decorative wrap-around porches
- Enhanced building materials and accents
- Courtyards visible from the street side elevation
- Single-story elements
- Roof plane breaks
- Secondary entry and/or garage access on street side yard
- Enhanced window treatments

“High Visibility” Elevations: Elevations with “High Visibility” are those that are visible from public or quasi-public areas, such as parks, streets, trails, and shopping centers. Elevations with “High Visibility” shall include the following design characteristics:

- The same amount of articulation on the visible portions of “High Visibility” elevations as on the front elevations.
- The use of a one-story element.
- A variety of architectural elements, such as window treatments, roof projections, wall plane variations and details.
- Roof details such as knee braces, rafter tails, and cornices to provide variation (when appropriate to the architectural style).
- Additionally, any residential façade that faces the street shall be constructed of masonry.

“Low Visibility” Elevations: The elevations that are only visible from neighboring Emerald Acres residential properties (interior lots) are the “Low Visibility” Elevations. These elevations do not require detailed articulation; however, elevations with “Low Visibility” shall include the following design characteristics:

- Window trim, where appropriate to style.
- Varied massing rather than a singular wall plane without breaks.

Residential Massing and Setbacks

Varied residential massing creates attractive and comfortable street scenes. Side yard setbacks and garage setbacks shall be varied whenever possible to achieve visual interest from one residential unit to the next within the same neighborhood. Residential units shall be reversed so that garages and primary entrances are adjacent to one another; however, this pattern should occasionally be broken to avoid monotony.

Single-story elements are highly encouraged on all homes to add variety and interest to the street scene. Single-story elements also create a pedestrian-scale, visually attractive streetscape. Examples of single-story elements include: a single-story room of interior living space, porches and courtyards (when appropriate to the architectural style of the building), and an extended entry alcove.

Residential Building Materials and Colors

Exterior building materials shall be of indigenous looking materials which are consistent with and reflect the natural character of the surrounding hillside environment. This includes wood, brick, masonry, concrete, stucco, or plaster and stone. Additionally, prefabricated stone and wood-like products are acceptable for use as an exterior building material. As previously described, any residential façade that faces the street shall be constructed of masonry.

Color is intended as a primary thematic element of the community. Consequently, building material colors should be compatible with the indigenous elements of the environment. Darker or lighter accents are encouraged to highlight the character of the structure; bright or non-earth tone colors should be avoided except as accents. Monochromatic color schemes are not allowed and buildings are encouraged to have at least three colors for the body, wainscoting, trim, and fascia. Colors selected should apply to the styles they represent.

Building materials and colors are important elements when used to achieve a true representation of a specific architectural style. Each structure should have a different predominant façade material or color than the immediately adjacent structure to promote individuality within the neighborhood context. The residential units within Emerald Acres shall utilize various siding materials to produce effects of texture and relief that provide character and consistency appropriate to the architectural style. Any changes in material should occur at changes in plane and foster an appearance that is substantial, integral, and complete. Exterior finish materials such as metal or aluminum siding and roofing, reflective materials, and unfinished concrete block are not acceptable. Building materials and colors shall complement the natural, climatic, and built environment of Emerald Acres. Whenever possible, materials should be durable and require minimal maintenance.

Paints, stains, and stucco should, in most cases, be limited primarily to muted or neutral colors, grays, and earth tones. Color schemes should be appropriate to the architectural concept chosen. Accent colors and pure hues should be used on moldings, doors, window frames, fascias, awnings, window boxes, shutters, cornices, and accent trim. Wood may be treated with transparent stains or paints if desired. Color palettes for each tract indicating a minimum of three colors per home is required, with a minimum of five palettes provided to achieve a variety of colors throughout each tract. All flashing and sheet metal should be painted or covered from view in a manner consistent with the general exterior architectural treatment of the building. Architectural screens, fences, and accessory structures should be compatible in material, color, and texture to the main buildings.

Roof Materials and Colors

A variety of roof styles shall be included within each neighborhood. Roof articulation should utilize vertical and horizontal articulation to introduce additional shapes, angles, and shadows to add visual relief to the tops of buildings. Examples of such roof articulation include gables, hips and dormers, and roof overhangs.

Steeper pitched roofs may be used as accent roofs to complement the street scene. Flat roofs and “A”-frame type roofs are prohibited. Finish roof flashings, rain gutters and downspouts, vents and other roof protrusions should match adjacent finish materials and/or colors. Roof additions shall be of the same materials as main structures.

Roof shingles and other roofing elements shall be made of fire retardant materials for all homes. Unfinished galvanized metal is not acceptable. Roof vents and accessories shall be positioned away from the street and/or finished to match the roof color to minimize visual impact. Skylights may be incorporated into the roof design to provide natural light and passive solar energy. Frame color should blend with the surrounding roof color, and aluminum frames are not allowed.

Within each neighborhood, variation in roof forms, colors, textures, and materials shall be utilized to create an appearance of individuality and avoid a uniform appearance. Roof pitches and forms may vary in order to encourage individual architectural expression. To provide for a reduced scale along the street, single story plate lines along front and side yard setbacks are encouraged.

Garage Placement and Treatment

The visual impact of the garage should be minimized to prevent a repetitious and monotonous appearance along the street. Garages shall feature a variety of placements, setbacks, and treatments. Additionally, garages shall adhere to the following design criteria:

- Neighborhoods shall have a variety of garage configurations including, but not limited to: swing-in, shallow-recessed, forward, drive-through, flush, tandem, split, and deep-recessed garages. Not all varieties need be used in any neighborhood or in the community.
- Garage doors should be compatible with the architectural style of the residence.
- Tandem garages may be used as parking for cars or for storage of boats, ATVs, Recreational Vehicles (RVs) or other accessory vehicles.
- All garage doors shall be roll-up doors or another type of upgraded door.
- All garage doors shall be recessed 12 inches and provide for lighting that complements the style of the home.
- There shall be a minimum of two separate, recessed garage doors for non-tandem three-car garages.

Outdoor Living Spaces

In many historical architectural styles, front porches and courtyards were considered outdoor living spaces. These outdoor living spaces evolved to become readily identifiable components of the historical architectural styles and have been incorporated into modern interpretations of the style. Outdoor living spaces are an integral part of the design of homes within Emerald Acres and should function as the transition between the interior living area and the outdoors. Porches, courtyards, and other outdoor living spaces must be compatible to the overall architectural style of the home.

In addition, when provided, porches within Emerald Acres shall conform to the following standards:

- The plate height of the porch shall not exceed the adjacent first-story plate height of the home, or 12 feet maximum, whichever is less.
- Columns, porch post supports and porch railings shall complement the architectural style of the home.
- Porches shall be a minimum of 6' deep.

- Encourage porches to wrap around the corner edge of homes located on corner lots.

Courtyards within Emerald Acres shall conform to the following standards:

- Courtyards shall be considered in the development plan for both attached and detached homes.
- Courtyard placement shall differ between plans.
- The courtyard wall shall not exceed thirty-six inches in height.
- Encourage courtyards to wrap around the corner edge of homes located on corner lots.

Windows and Doors

The design of doors and windows must possess visual integrity and be appropriately scaled to the mass and scale of the residential unit. Windows and doors shall also add function and aesthetic value to the house. These elements shall be distinctive and appropriate to other elements of the architectural style (such as shutters, patterns, trim, and colors). Window and door detailing must occur on “High Visibility” elevations to avoid the appearance of blank walls.

Windows on residential units throughout Emerald Acres shall include the following design characteristics:

- Awnings and trellises, when applied, should be used on south-facing and west-facing façades to provide shade.
- Shutters should be consistent with the general architectural style of the unit.

Doors on residential units throughout Emerald Acres shall include the following design characteristics:

- Emphasis should be placed on the design and type of entry door used. Entry doors function as an introduction to the interior of the unit and concern should be given to the image the door creates.
- The entire door assembly should be treated as a single design element including surrounding frame, molding, and side lights.
- Both single and double doors are appropriate. The door should typically be covered by an overhead element, such as a porch, or recessed into the wall plane.
- Flexibility is allowed concerning the color of the door. It may match or contrast with the accent trim, but should be differentiated from the wall color.

Lighting

Lighting shall create a unified natural effect that will not interfere or compete with the site and surrounding land uses. All lighting may be shielded to reduce glare and arranged to reflect lights away from residential units. To preserve the night sky, the following guidelines have been established:

- Lighting must be indirect with the lighting source preferably not visible.
- Ornamental or accent lighting may be fully shielded, hooded, and directed downward to avoid spilling onto adjacent residences and may not illuminate beyond the limits of the property.
- Exposed lights, spot lights, lenses and reflectors are prohibited.
- Use of colored light sources and glass is not allowed.
- Porch and decorative lighting must compliment the architectural style of the unit.

Accessory Structures

Accessory structures (cabanas, storage sheds, etc.) shall have roofs that are similar and/or compatible with the roof of the associated residential dwelling unit. The design of accessory structures shall be compatible with the main structure through the use of architecture, fence connections and/or landscaping. There is no minimum roof pitch required for accessory structures. Flat roofs on accessory structures are permitted in all zones in Emerald Acres.

Mailboxes

Installation of cast iron, cast aluminum, brick, or slumpstone-encased curbside mailboxes is required. Final type and location of mailboxes will be coordinated with United States Postal Service official at the tentative map level.

Mechanical Equipment

All air conditioning/heating equipment, soft water tanks, pool and spa equipment, electric self-timer boxes for irrigation or lighting, or any other form mechanical equipment shall be screened from public view. Roof-mounted mechanical equipment such as air conditioning units are not permitted unless screened by material that complements the architectural style of the building in design, color, and material. Sound attenuation is also encouraged. Mechanical equipment may encroach up to two feet in the side yard setback and up to four feet into the rear setback.

Solar panels, if used, shall be integrated into the roof design as an unobtrusive element. Panels are to be parallel to the roof plane and should be clear, bronze, or smoke-colored plastic or glass. To the greatest extent possible, solar panels, satellite dishes, and other similar roof-mounted mechanical equipment should be located away from front elevation street views.

Accessory Vehicle Parking

Off-site storage and parking of accessory vehicle (including, but not limited to boats, RVs, and ATVs) is highly encouraged. On-site storage and parking of any accessory vehicle is allowed; however, these vehicles are not permitted in the front or side yard of any homes within the Emerald Acres community. All accessory vehicles shall either be stored in the garage or in the rear of home. In all cases, the accessory vehicle shall not be visible from the street.

4.3.1.2 SINGLE-FAMILY RESIDENTIAL ARCHITECTURAL STYLES

The types and architectural styles of the homes within Emerald Acres reinforce the community's design theme and reflect the architectural themes and styles prevalent throughout southern California. The selected architectural styles for Emerald Acres include Craftsman, Bungalow, Spanish, and Monterey.

These complementary home product types and architectural styles provide a wide range of architectural variation, appealing to a variety of potential homeowners and creating visually interesting streetscenes. The design goal of Emerald Acres is to achieve contemporary interpretations of traditional home products and historical architectural styles rather than exact recreations. As such, these Design Guidelines are intended to present images of key features and details representative of the selected home products and architectural styles that should be incorporated into the homes within Emerald Acres.

Representative architectural styles, details, and home product elevations for Single-Family Detached homes that are encouraged within the Emerald Acres community are shown on Exhibit 4-21 through Exhibit 4-31.

4.3.2 RESIDENTIAL DESIGN GUIDELINES –VILLAGE 4

Either attached or detached residences arranged in a motorcourt configuration are provided within Village 4 and shall reflect an architectural style consistent with the character of the single-family neighborhoods throughout the Emerald Acres community. The attached and detached motorcourt homes within Village 4 shall be designed to utilize either Craftsman, Bungalow, Spanish, or Monterey styles.

4.3.2.1 VILLAGE 4 RESIDENTIAL ARCHITECTURAL ELEMENTS

The guidelines in this section address important residential architectural elements, such as roofs, visible edges, garage placement, and outdoor living areas. Together, these key elements can facilitate the creation of diverse and varied streetscapes while also maintaining a cohesive sense of place. This section contains general requirements that apply the development of attached and detached motorcourts within Village 4.

The successful development of a pedestrian-friendly and family-oriented neighborhood within Village 4 can be achieved by implementing the following goals:

- Provide innovative plans by avoiding repetitious designs, massing, roof forms, colors, elevations and footprints.
- Variations in floor plans shall be provided to ensure one unit's floor plan does not simply mirror the adjacent unit's, wherever practical.
- Provide architectural styles that complement building form.
- Incorporate architectural details along visible edges.
- Encourage the integration of indoor and outdoor living spaces.
- Design the composition of building mass with integrity and character.
- Provide interesting, pedestrian-scale architectural elements along street frontages.

Visible Edges

In order to ensure that residential development within Village 4 is visually pleasing, all visible edges of attached and detached motorcourt buildings within the community are subject to a certain degree of articulation.

Roofs: Rows of motorcourt buildings seen from a distance or along arterial roads are perceived in relation to their contrast against the skyline or background. Therefore, the primary visual impact from this viewpoint is the shape of the building and roofline. Providing a variety of roof forms can help avoid a monotonous conditions.

“High Visibility” Elevations: As described earlier, elevations with “High Visibility” are those that are visible from public or quasi-public areas, such as parks, streets, trails, and shopping centers. Elevations with “High Visibility” shall include the following design characteristics:

- The same amount of articulation on the visible portions of “High Visibility” elevations as on street-side elevations.
- A variety of architectural elements, such as window treatments, roof projections, wall plane variations and details.

- Roof details such as knee braces, rafter tails, and cornices to provide variation (when appropriate to the architectural style).
- Additionally, any residential façade that faces the street shall be constructed of masonry.

“Low Visibility” Elevations: As described earlier Elevations with “Low Visibility” are those that are only visible from neighboring motorcourt buildings. These elevations do not require detailed articulation; however, elevations with “Low Visibility” shall include the following design characteristics:

- Window trim, where appropriate to style.
- Varied massing rather than a singular wall plane without breaks.

Building Massing and Setbacks

Street-side frontages of attached and detached motorcourt buildings shall be architecturally articulated through the use of windows, protections, balconies, and other decorative elements appropriate to the structure’s architectural style. The internal frontages that face neighboring motorcourt buildings are also subject to architectural articulation, but to a reduced degree, in order to ensure an aesthetic appearance from all viewpoints.

Residential Building Materials and Colors

Exterior building materials shall be of indigenous-looking materials which are consistent with and reflective of the natural character of the surrounding hillside environment. This includes wood, brick, masonry, concrete, stucco, or plaster and stone. Additionally, prefabricated stone and wood-like products are acceptable for use as an exterior building material.

Color is intended as a primary thematic element of the community. Consequently, building material colors should be compatible with the indigenous elements of the environment. Darker or lighter accents are encouraged to highlight the character of the structure; bright or non-earth tone colors should be avoided except as accents. Monochromatic color schemes are not allowed and buildings are encouraged to have at least three colors for the body, wainscoting, trim, and fascia. Colors selected should apply to the styles they represent.

Building materials and colors are important elements when used to achieve a true representation of a specific architectural style. Each structure should have a different predominant façade material or color than the immediately adjacent structure to promote individuality within the neighborhood context. The attached and detached motorcourt buildings within Village 4 shall utilize various siding materials to produce effects of texture and relief that provide character and consistency appropriate to the architectural style. Any changes in material should occur at changes in plane and foster an appearance that is substantial, integral, and complete. Exterior finish materials such as metal or aluminum siding and roofing, reflective materials, and unfinished concrete block are not acceptable. Building materials and colors shall complement the natural, climatic, and built environment of Emerald Acres. Whenever possible, materials should be durable and require minimal maintenance.

Paints, stains, and stucco should, in most cases, be limited primarily to muted or neutral colors, grays, and earth tones. Color schemes should be appropriate to the architectural concept chosen. Accent colors and pure hues should be used on mouldings, doors, window frames, fascias, awnings, window boxes, shutters, cornices, and accent trim. Wood may be treated with transparent stains or paints if desired. Color palettes for each tract indicating a minimum of three colors per home is required, with a minimum of five

palettes provided to achieve a variety of colors throughout each tract. All flashing and sheet metal should be painted or covered from view in a manner consistent with the general exterior architectural treatment of the building. Architectural screens, fences, and accessory structures should be compatible in material, color, and texture to the main buildings.

Roof Materials and Colors

A variety of roof styles may be included within Village 4. Roof articulation should utilize vertical and horizontal articulation to introduce additional shapes, angles, and shadows to add visual relief to the tops of buildings. Examples of such roof articulation include gables, hips and dormers, and roof overhangs. Steeper pitched roofs may be used as accent roofs to complement the street scene. Flat roofs and “A”-frame type roofs are prohibited. Finish roof flashings, rain gutters and downspouts, vents and other roof protrusions should match adjacent finish materials and/or colors. Roof additions shall be of the same materials as main structures.

Roof shingles and other roofing elements shall be made of fire retardant materials for all homes. Unfinished galvanized metal is not acceptable. Roof vents and accessories shall be positioned away from the street and/or finished to match the roof color to minimize visual impact. Skylights may be incorporated into the roof design to provide natural light and passive solar energy. Frame color should blend with the surrounding roof color, and aluminum frames are not allowed.

Within Village 4, variation in roof forms, colors, textures, and materials may be utilized to create an appearance of individuality and avoid a uniform appearance. Roof pitches and forms may vary in order to encourage individual architectural expression. To provide for a reduced scale along the street, single story plate lines along front and side yard setbacks are encouraged.

Garage Placement and Treatment

The visual impact of the garage should be minimized to prevent a repetitious and monotonous appearance within the motorcourt or along the streets. Garages should feature a variety of placements, setbacks, and treatments. Additionally, garages shall adhere to the following design criteria:

- Garage doors should be compatible with the architectural style of the residence.
- All garage doors shall be roll-up doors or another type of upgraded door.

Windows and Doors

The design of doors and windows must possess visual integrity and be appropriately scaled to the mass and scale of the attached and detached motorcourt buildings, and should be carefully situated to ensure privacy between units. Windows and doors shall also add function and aesthetic value to the building. These elements shall be distinctive and appropriate to other elements of the architectural style (such as shutters, patterns, trim, and colors). Window and door detailing must occur on “High Visibility” elevations to avoid the appearance of blank walls.

Windows on attached and detached motorcourt buildings throughout Village 4 shall include the following design characteristics:

- Awnings and trellises, when applied, should be used on south-facing and west-facing façades to provide shade.
- Shutters should be consistent with the general architectural style of the unit.

Doors on attached and detached motorcourt buildings throughout Village 4 shall include the following design characteristics:

- Emphasis should be placed on the design and type of entry door used. Entry doors function as an introduction to the interior of the unit and concern should be given to the image the door creates.
- The entire door assembly should be treated as a single design element including surrounding frame, molding, and side lights.
- The door should typically be covered by an overhead element, such as a porch, or recessed into the wall plane.
- Flexibility is allowed concerning the color of the door. It may match or contrast with the accent trim, but should be differentiated from the wall color.

Lighting

Lighting shall create a unified natural effect that will not interfere or compete with the site and surrounding land uses. All lighting may be shielded to reduce glare and arranged to reflect lights away from residential units. To preserve the night sky, the following guidelines have been established:

- Lighting must be indirect with the lighting source preferably not visible.
- Ornamental or accent lighting shall be fully shielded, hooded, and directed downward to avoid spilling onto adjacent residences and may not illuminate beyond the limits of the property.
- Exposed lights, spot lights, lenses and reflectors are prohibited.
- Use of colored light sources and glass is not allowed.
- Security lighting within common areas must compliment the architectural style of the motorcourt building.

Mailboxes

Installation of cast iron, cast aluminum, brick, or slumpstone-encased community mailboxes is required. Final type and location of community mailboxes will be coordinated with United States Postal Service official at the tentative map level.

Mechanical Equipment

All air conditioning/heating equipment, soft water tanks, electric self-timer boxes for irrigation or lighting, or any other form mechanical equipment shall be screened from public view. Roof-mounted mechanical equipment such as air conditioning units are not permitted unless screened by material that complements the architectural style of the building in design, color, and material. Sound attenuation is also encouraged.

Solar panels, if used, shall be integrated into the roof design as an unobtrusive element. Panels are to be parallel to the roof plane and should be clear, bronze, or smoke-colored plastic or glass. To the greatest extent possible, solar panels, satellite dishes, and other similar roof-mounted mechanical equipment should be located away from front elevation street views.

4.3.2.2 VILLAGE 4 RESIDENTIAL ARCHITECTURAL STYLES

The types and architectural styles of the attached and detached motorcourt buildings within Emerald Acres reinforce the community's design theme and reflect the architectural themes and styles prevalent throughout southern California. The selected architectural styles for Emerald Acres include Craftsman, Bungalow, Spanish, and Monterey.

These complementary home product types and architectural styles provide a wide range of architectural variation, appealing to a variety of potential homeowners and creating visually interesting street scenes. The design goal of Emerald Acres is to achieve contemporary interpretations of traditional home products and historical architectural styles rather than exact recreations. As such, these Design Guidelines are intended to present images of key features and details representative of the selected home products and architectural styles that should be incorporated into the homes within Emerald Acres.

Representative architectural styles, details, and home product elevations for attached and detached motorcourt homes are shown on Exhibit 4-21 through Exhibit 4-28 and on Exhibit 4-32, Village 4 – Typical Product Elevations – Attached Motorcourts, and Exhibit 4-33, Village 4 – Typical Product Elevations – Detached Motorcourts.

4.3.3 COMMERCIAL DESIGN GUIDELINES – PLANNING AREA 13

The design and relationship between structures, public spaces, and the pedestrian environment plays an important role in creating an innovative retail area with a welcoming, positive atmosphere. The layout of the Emerald Acres commercial Planning Area not only influences the social experience of users of the retail area, but also how passersby on nearby roadways view the community. As such, the design of building sites within the retail area is an essential component of the Emerald Acres Specific Plan.

Commercial buildings within Planning Area 13 should be oriented in a manner that is complementary to the community's overall layout (including streets) and architectural composition and compatible with nearby development. In addition, the retail area should be designed in a manner that is considerate of its visibility from public viewing areas. The retail area is visible to vehicles traveling on Florida Avenue, which is highly beneficial to the businesses located within the retail area; however, the public prominence of these structures as viewed from this roadway requires that special care be taken to ensure these commercial buildings are visually appealing and sited in a manner that reduces the perceived mass of structures. Alternatively, building facades should include articulation and detailing that creates visual interest when viewed from Florida Avenue. Buildings shall be clustered together to create gathering places with site amenities, and to facilitate efficient vehicular and pedestrian access. Site planning concepts for the neighborhood shopping center are illustrated on Exhibit 3-11, Planning Area 13 Development Standards whereas representative imagery demonstrating appealing architectural elements are shown on Exhibit 4-34, Planning Area 13 Commercial Concept Images.

The principles and guidelines are not intended to restrict taste or preference, but are designed to avoid harsh contrasts and to foster harmonious development throughout the Emerald Acres community. Commercial buildings should be designed as a complementary addition to the overall theme established throughout the community and should acknowledge and respect existing nearby structures and public spaces. Dramatic departures in form, scale, and architectural style are not permitted.

- Buildings should be designed to create smooth transitions in scale through the use of low and mid-rise building forms or terraced elevations.
- Buildings should be clustered to define public spaces and to create gathering places with site amenities.

- Parking areas should be divided into a series of smaller connected lots separated by additional landscaping.
- Parking lots should generally be placed away from Florida Avenue, preferably behind buildings.
- Different uses should be interconnected to the pedestrian circulation system through the use of walkways, hardscape, and landscape.
- Prominent and/or unique architectural elements should be positioned as the focal points in the development.
- One and two-story building massing should occur. False second-story elements can be incorporated to provide for variation in building massing and reinforce the enclosure of public spaces and plazas.
- Storefront signage is required along rear facades facing onto parking lots and/or public spaces as well as front facades facing streets and plazas.
- Major building entries should be emphasized with special massing and/or architectural treatment.
- Provide safe and convenient vehicular and pedestrian circulation patterns within and between developments.
- Where possible, separate service and delivery areas from parking areas.
- Long, uniform façades should be avoided by creating visual interest through the use of courtyards, varied building setbacks, arcades, windows and towers.
- A focal point of special interest should be presented to the public view.
- Buildings should be oriented and designed to locate high activity uses, such as restaurants and entertainment facilities, adjacent to major pedestrian ways.
- All lighting and illuminated signs shall be located and designed in such a way to confine illumination to the premises.
- The concept for commercial architecture should be consistent with the overall theme and architectural character of the residential portion of the community.



Essential Elements of the Craftsman Style

1. Shallow pitched roofs with deep overhangs.
2. Deep, broad porch element with expressive structural components such as square, or tapered columns.
3. Expressive structural elements such as rafters, brackets, braces and columns.
4. A mixture of materials such as stone, shingles and wood siding.
5. Asymmetrical massing and window and door compositions.

History and Character

The Craftsman Style, an American architectural style, represented a philosophy of life that featured honesty, integrity and a return to nature. The Craftsman design focuses on the harmony of indoor and outdoor life. It stresses honesty of form, materials and workmanship, eschewing applied decoration in favor of the straight forward expression of the structure. Craftsman architecture draws from the wood building traditions of Japan and Switzerland, as well as medieval themes favored by the Arts and Crafts philosophies. Natural woods, shingles, earth tone colors, brick, stone, river rock, clinker brick, and heavy structural beams signify oneness with nature. Rocks and bricks are often used on foundations, chimneys, and railings to set a unifying theme for the home. Wherever possible, aesthetic and functional interiors are integrated in simple living spaces. These easy, asymmetrical gabled, stuccoed works of art are a large part of Southern California's architectural heritage.



EXHIBIT 4-21. CRAFTSMAN ARCHITECTURAL STYLE



Wood and shingle siding, broad porches, and exposed rafter tails all spell the Craftsman theme



Wood-beamed, decorative gables



Complex window and door compositions with wood detailing



Garage door treatments echo the Craftsman style through use of wood framing, windows, and lighting fixtures



EXHIBIT 4-22. CRAFTSMAN ARCHITECTURAL DETAILS



Essential Elements of the Bungalow Style

1. Flat composition tile roof
2. Vertical multi-paned windows
3. Tapered stucco porch columns
4. Vertical detail at gables

History and Character

The Bungalow Style is an eclectic style comprised of many different influences, notably Spanish, Ranch, and Craftsman.

The exterior material is stucco with siding and shutter accents. The massing is either symmetrical or asymmetrical. Porches, if present, feature tapered stucco posts. The roof is usually constructed of composition tile (flat or shingle) and subordinate gables may include vertical siding detail.

Other details that may be featured as part of the Bungalow architectural style include vertical window mullion patterns, accent potshelves, accent shutters, and ganged window massing with foam trim accents below the windows.



EXHIBIT 4-23. BUNGALOW ARCHITECTURAL STYLE



Flat composition tiled roof with vertical details at the gables

A mixture of materials such as stucco and wood siding create a very appealing porch transition with tapered columns

Vertical multi-paned windows

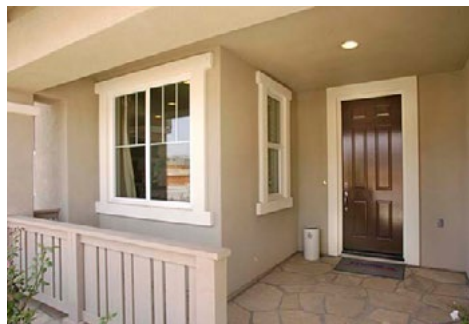


EXHIBIT 4-24. BUNGALOW ARCHITECTURAL DETAILS



Essential Elements of the Spanish Style

1. Stucco exterior walls with smooth to light sand finish
2. Flat clay tile or concrete “S” tile on predominantly hip roofs
3. Wood fascias, rafters, and soffit brackets
4. Segmented or elliptical arch shape used as the characteristic shape with divided lites
5. Wrought iron decorative details
6. Ornamental wood details such as small balconies

History and Character

The inspiration for The Spanish Style dates back as far as the late 17th Century. Spanish and Mexican missionaries settling in the southwest set up the small communities known as missions to convert the American Indians into Catholicism.

The primary structure was typically the church, forming one of the four outside walls of the buildings with an interior ‘corridor’ or veranda surrounding a central courtyard. This plan form not only protected its residents from Indian attacks, but provided a pleasant respite from the heat encompassing areas for patio and garden. Using the materials and labor at hand combined with the influences from Spain and other parts of Europe, these buildings took on a unique regional appearance.

The American Indian influence produced a simple flat walled building like a pueblo with heavy wooden gates and few other openings, built with adobe, wood and tile, then plastered. The corners, worn by the environment, took on a soft rounded look, not too different than some of the purposely sculpted forms from Spain. The sculpted parapet walls, arched forms, bell towers and sometimes ornate detail at the entry came from the Spanish influence. Later, wood railings, decorative wooden gates and shutters were added and as this style evolved, became more ornamental details.



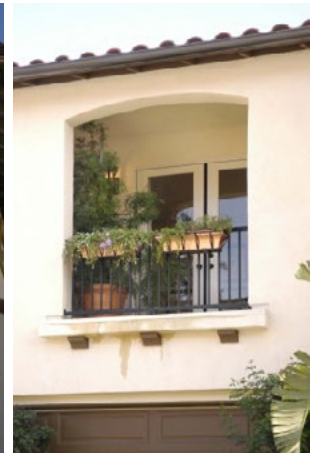
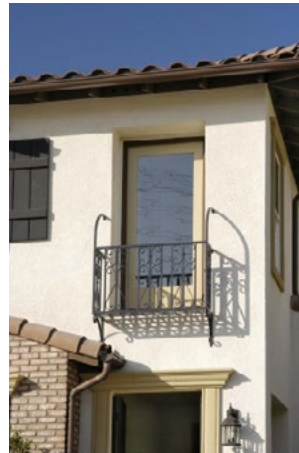
EXHIBIT 4-25. SPANISH ARCHITECTURAL STYLE



Arched wood front door



Arched windows



Wood or metal shutter details, clipped eaves, and deep ledged windows



Sculpted stucco and wrought iron ornamentation



Mix of stucco and tile

EXHIBIT 4-26. SPANISH ARCHITECTURAL DETAILS



Essential Elements of the Monterey Style

1. Second-story cantilevered balcony
2. Simple forms
3. Low-pitched gable roofs
4. Wood shutters
5. Vertical form and multi-paned windows

History and Character

The Monterey Style is a revival of the Spanish Colonial houses of northern California, blending adobe construction with English shapes from New England.

This style primarily utilizes second-story balconies, which are usually cantilevered and covered by the principal roof. Balconies are generally not located above living areas. Monterey style homes are typically characterized by simple house forms, relatively low-pitched hip or gable roofs, and wide overhangs. Walls convey a thick appearance with recessed door and window openings.

Shutters, balconies, verandas, and porches are indicative of the Monterey architectural style. Features such as arches, courtyards, and colonnades are used frequently. The first and second stories may have different materials, with wood siding above and a stucco or brick veneer base below. Large-scale chimneys are often present.



EXHIBIT 4-27. MONTEREY ARCHITECTURAL STYLE



Arched doors



Single garage doors contribute to this styling



The elevation adheres to the thematic principles of the styling by mixing brick and an expansive balcony.



The wood detailing of the balcony and rafter tails is a strengthening attribute



Shutter detailing and ornate window elements

EXHIBIT 4-28. MONTEREY ARCHITECTURAL DETAILS



EXHIBIT 4-29. VILLAGE 1 - TYPICAL PRODUCT ELEVATIONS



EXHIBIT 4-30. VILLAGE 2 - TYPICAL PRODUCT ELEVATIONS



EXHIBIT 4-31. VILLAGE 3 - TYPICAL PRODUCT ELEVATIONS



EXHIBIT 4-32. VILLAGE 4 - TYPICAL PRODUCT ELEVATIONS - ATTACHED MOTORCOURTS



EXHIBIT 4-33. VILLAGE 4 - TYPICAL PRODUCT ELEVATIONS - DETACHED MOTORCOURTS



EXHIBIT 4-34. PLANNING AREA 13 - COMMERCIAL CONCEPT IMAGES

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NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. **Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org.** The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. For more information please contact **ALUC Planner Jackie Vega at (951) 955-0982.**

The County of Riverside Planning Department should be contacted on non-ALUC issues. For more information please contact County of Riverside Planner Russell Brady at (951) 955-3025.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to javega@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please contact Barbara Santos at (951) 955-5132.

PLACE OF HEARING: **Riverside County Administration Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside California**

DATE OF HEARING: **October 12, 2023**

TIME OF HEARING: **9:30 A.M.**

CASE DESCRIPTION:

ZAP1072HR23 – Peak Emerald Acres, LLC (Representative: Sorrento Advisors) – County of Riverside Case Nos. GPA01111 (General Plan Amendment), SP381 (Specific Plan), CZ7774 (Change of Zone), TTM36452 (Tentative Tract Map), a proposal to establish the Emerald Acres Specific Plan allowing for a maximum of 391 residential dwelling units on 335 acres, 4.59 acres of commercial space, 6.6 acres of parkland, and 191.3 acres of conserved open space, located southerly of Florida Avenue, easterly of Calvert Avenue, westerly of California Avenue, and northerly of Stowe Road. The applicant also proposes to amend the General Plan land use designation of the site from Commercial Retail (CR), Low Density Residential (LDR), Medium High Density Residential (MHDR), Rural Mountainous (RM) to the land uses as reflected in the Specific Plan including Medium Density Residential (MDR), High Density Residential (HDR), Commercial Retail (CR), Open Space – Recreation (OS-R), Open Space – Conservation Habitat (OS-CH), Open Space – Conservation (OS-C), and Public Facilities (PF), and change the zoning of the site from Heavy Agriculture 10 Acre Minimum (A-2-10) and Controlled Development Areas (W-2) to Specific Plan and establishing a Specific Plan zoning ordinance that will include the permitted uses and development standards for each Planning Area of the Specific Plan, and establish the Planning Area boundaries of the Specific Plan. The applicant also proposes to divide the 335 acre site into specific lot configurations within each village area to allow for proposed development and resource preservation (Airport Compatibility Zones D and E of the Hemet-Ryan Airport Influence Area)



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC STAFF ONLY

ALUC Case Number: _____

Date Submitted: _____

AIA: _____

Zone: _____

Public Hearing _____

Staff Review _____

Applicant

Applicant

Full Name: _____

Applicant Address: _____

Phone: _____ Email: _____

Representative/ Property Owner Contact Information

Representative: _____ Email: _____

Phone: _____

Address: _____

Property Owner: _____ Email: _____

Phone: _____

Address: _____

Local Jurisdiction Agency

Agency Name: _____ Phone: _____

Staff Contact: _____ Email: _____

Address: _____ : _____ :

Local Agency Case No.: _____

Project Location

Street Address: _____ Gross Parcel Size.: _____

Assessor's Parcel No.: _____

Solar

Is the project proposing solar Panels? Yes

No

If yes, please provide solar glare study.
(only if in Zone C or higher)

Data

Site Elevation:(above
mean sea level) _____

Height of Building or
structures: _____

What type of drainage basins are
being proposed and the square
footage: _____

Notice

A. NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. REVIEW TIME: Estimated time for “staff level review” is approximately 30 days from date of submittal. Estimated time for “commission level review” is approximately 45 days from date of a complete application submittal to the next available commission hearing meeting.

C. SUBMISSION PACKAGE:

Please submit all application items DIGITALLY via USB or CD:

- Completed ALUC Application Form
- Plans Package: site plans, floor plans, building elevations, grading plans, subdivision maps
- Exhibits of change of zone, general plan amendment, specific plan amendment
- Project description of existing and proposed use

Additionally, please provide:

- ALUC fee payment (Checks made out to Riverside County ALUC)
- Gummed address labels of all surrounding property owners within a 300-foot radius of project site. (Only required if the project is scheduled for a public hearing).

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM: 3.2

HEARING DATE: October 12, 2023

CASE NUMBER: ZAP1128FV23 – Xebec Building Company (Representative: Casc Engineering & Consulting, Inc.)

APPROVING JURISDICTION: County of Riverside

JURISDICTION CASE NO: GPA23-0004 (General Plan Amendment), SP00265A02 (Specific Plan Amendment), CZ230012 (Change of Zone), and PPT23-0002 (Plot Plan)

LAND USE PLAN: 2007 French Valley Airport Land Use Compatibility Plan, as amended in 2011

Airport Influence Area: French Valley Airport

Land Use Policy: Compatibility Zones B2 and D

Noise Levels: Between 60-65 CNEL from aircraft noise

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends that the Commission find the proposed, General Plan Amendment, Specific Plan Amendment, and Change of Zone CONSISTENT with the 2007 French Valley Airport Land Use Compatibility Plan, as amended in 2011, and find the proposed Plot Plan CONDITIONALLY CONSISTENT, subject to the conditions included herein, and such additional conditions as may be required by the Federal Aviation Administration Obstruction Evaluation Service.

PROJECT DESCRIPTION: A proposal to construct five industrial buildings totaling 347,100 square feet with mezzanines and two commercial buildings totaling 64,000 square feet with mezzanines on a 19.61 acre parcel. The applicant also proposes amending the General Plan to change the sites land use designation from commercial retail and commercial office to light industrial. The applicant also proposes amending the sites Specific Plan land use designation from Commercial Retail and Commercial office to Light Industrial. The applicant proposes changing the sites zoning to update the Borel Airpark Specific Plan adopted ordinance to reflect the proposed land uses.

PROJECT LOCATION: The proposed project is located northerly of KMT way, southerly of Sparkman Way, easterly of Winchester Road, and westerly of Sky Canyon Drive, approximately 2,639 feet southwesterly of the southerly terminus of Runway 18-36 at French Valley Airport.

BACKGROUND:

Non-Residential Average Land Use Intensity: Pursuant to the French Valley Airport Land Use Compatibility Plan, the project boundary is located within Zones B2 (9.36 acres) and Zone D (10.25 acres), which limits average intensity to 100 people per acre in Zone B2, and 150 people per acre in Zone D.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, the following rates were used to calculate the occupancy for the proposed buildings:

- Office – 1 person per 200 square feet
- Manufacturing – 1 person per 200 square feet
- Retail- 1 person per 115 square feet

The project proposes to construct five warehouse buildings with mezzanines totaling 347,100 square feet and two commercial retail buildings totaling 64,000 square feet with mezzanines, on 19.61 acres. An individual zone by zone analysis is provided below.

- Within Zone B2 (9.36 acres) the project includes from Building 1: 50,000 square feet of manufacturing area, 8,000 square feet of first floor office area, and 8,000 square feet of second floor office mezzanine area; Building 2: 10,900 square feet of manufacturing area; Building 3: 8,358 square feet of manufacturing area; Building 4: 70,800 square feet of manufacturing area, 8,000 square feet of first floor office area, and 8,000 square feet of second floor mezzanine area, accommodating a total occupancy of 860 people, resulting in an average intensity of 92 people per acre, which is consistent with the Compatibility Zone B2 average intensity criterion of 100 people per acre.
- Within Zone D (10.25 acres) the project includes Building 2: 58,100 square feet of manufacturing area, 8,000 square feet of first floor office area, and 8,000 square feet of second floor office mezzanine area; Building 3: 47,642 square feet of manufacturing area, 8,000 square feet of first floor office area, and 8,000 square feet of second floor mezzanine area; Building 5: 31,300 square feet of manufacturing area, 3,000 square feet of first floor office area, and 3,000 square feet of second floor mezzanine area; Building 6: 26,000 square feet of retail area, and 6,000 square feet of second floor retail mezzanine area; Building 7: 26,000 square feet of retail area, and 6,000 square feet of second floor retail mezzanine area, accommodating 1,432 people, resulting in an average intensity of 140 people, which is consistent with the Compatibility Zone D average intensity criterion of 150 people per acre.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per standard vehicle). Based on the number of vehicle parking spaces provided (575), the total occupancy would be estimated at 863 persons, resulting in an average intensity of 44 persons per acre, which is consistent with the Zone B2 and D of average intensity criterion of 100 people per acre for Zone B2, and 150 people per acre for Zone D.

Non-Residential Single-Acre Intensity: Pursuant to the French Valley Airport Land Use Compatibility Plan, the project boundary is located within Zones B2 (9.36 acres) and Zone D (10.25 acres), which limits single acre intensity to 200 people per acre in Zone B2, and 450 people per

acre in Zone D.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre area for each of the buildings are as follows:

- Within Zone B2 the single acre intensity includes Building 1: 30,680 square feet of industrial area, 4,000 square feet of first floor office area, and 4,000 square feet of second floor office mezzanine area (an additional 4,880 square feet located outside the building), resulting in single acre intensity of 194 people, which is consistent with the Compatibility Zone B2 single acre intensity criterion maximum of 200 people.
- Within Zone D the single acre intensity includes Building 7: 18,973 square feet of retail area; Building 3: 6,993 square feet of industrial area, 1,300 square feet of first floor office area, and 1,300 square feet of second floor office mezzanine area (an additional 14,994 square feet located outside the building), resulting in a single acre intensity of 213 people, which is consistent with the Compatibility Zone D single acre intensity criterion maximum of 450 people.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zone B2.

Noise: The French Valley Airport Land Use Compatibility Plan depicts the site as being located within the 60 - 65 CNEL contour range from aircraft noise. Office and industrial uses are identified as normally acceptable within this range; however, staff is recommending a condition to incorporate noise attenuation measures into the design of the proposed office areas to such extent as may be required to ensure that interior noise levels from aircraft operations are at or below 45 CNEL.

Part 77: The elevation of Runway 18-36 at its southerly terminus is 1,330 feet above mean sea level. At a distance of approximately 2,639 feet from the runway to the site, Federal Aviation Administration (FAA) review would be required for any structures with a top of point elevation exceeding 1,356 feet AMSL. The site's finished floor elevation is 1,325 feet AMSL and the proposed structure height is 50 feet, for a top point elevation of 1,375 feet AMSL. Therefore, review of the building for height/elevation reasons by the FAA Obstruction Evaluation Service (FAAOES) was required. The applicant has submitted Form 7460-1, and FAA OES has assigned Aeronautical Study Nos. 2023-AWP-11049 thru 2023-AWP-11076-OE, to this project.

Determinations of No Hazard to Air Navigation letters were issued by the FAA OES on July 20, 2023, and it was determined that the buildings would not result in an impact to air navigation. The FAA OES conditions have been incorporated into ALUC's conditions.

Open Area: Pursuant to the French Valley Airport Land Use Compatibility Plan, the project site is located within Compatibility Zone B2 and D. The Compatibility Plan requires projects 10 acres or larger to designate 10% of project area for zone D as ALUC qualifying open area that could potentially serve as emergency landing areas. Zone B2 does not require open land.

Based on the project size (10.25 acres) located within Compatibility Zone D, the project is required to provide a minimum 1.025 acres of open area consistent with ALUC open area criteria. The applicant has provided a total of 1.44 acres of open area within the drive aisles and parking areas. These areas are conditioned to maintain a minimum shape of 75 feet in width and 300 feet in length

and shall be kept obstacle and obstruction free per ALUC open area definition (no objects greater than four feet in height with a diameter of four inches or greater).

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33C). The nearest portion of the project is located 2,639 feet from the runway, and therefore would be subject to the above requirement.

Although the nearest portion of the proposed project is located within 10,000 feet of the runway (approximately 2,639 feet), the project utilizes underground basins which will not contain surface water or attract wildlife and, therefore, would not constitute a hazard to flight.

Specific Plan Amendment/Change of Zone: The applicant also proposes amending the General Plan to change the sites land use designation from Commercial Retail and Commercial Office to Light Industrial. The applicant also proposes amending the sites Specific Plan land use designation from Commercial Retail and Commercial office to Light Industrial. Lastly, the applicant proposes changing the sites zoning to update the Borel Airpark Specific Plan adopted ordinance to reflect the proposed land uses. The proposed amendments would be consistent with the Compatibility Plan as long as the underlying development's intensity is consistent with the compatibility criteria.

CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, outdoor production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators).

- (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, day care centers, libraries, hospitals, nursing homes, places of worship, buildings with more than two aboveground habitable floors, critical community infrastructure facilities, and aboveground bulk storage of 6,000 gallons or more of flammable or hazardous materials.
 - (f) Highly noise-sensitive outdoor nonresidential uses.
 - (g) Any use which results in a hazard to flight, including physical (e.g., tall objects), visual, and electronic forms of interference with the safety of aircraft operations.
3. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property and be recorded as a deed notice.
 4. Prior to issuance of building permits, the landowner shall convey an avigation easement to the County of Riverside as owner of French Valley Airport or provide evidence that such easement has been previously conveyed. Contact the Riverside County – Aviation Division at (951) 955-9722 for additional information.
 5. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the stormwater basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

6. Noise attenuation measures shall be incorporated into the design of the building to the extent such measures are necessary to ensure that interior noise levels from aircraft operations are at or below 45 CNEL.
7. This project has been evaluated as consisting of five industrial buildings totaling 347,100 square feet with mezzanines and two commercial buildings totaling 64,000 square feet with

mezzanines all in one parcel on 19.61 acres. Any increase in building area (including construction of a new building), change in use to any higher intensity use, change in building location, or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.

8. At least 1.025 acres of ALUC-eligible open areas (at least 75 feet in width and 300 feet in length), as depicted on the Open Space exhibit, a copy of which is attached, shall be kept obstacle and obstruction free per ALUC open area definition (no objects greater than four feet in height with a diameter of four inches or greater).
9. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission.
10. The Federal Aviation Administration has conducted aeronautical studies of the proposed project (Aeronautical Study Nos. 2023-AWP-11049 thru 2023-AWP-11076-OE) and has determined that neither marking nor lighting of the structure is necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 M and shall be maintained in accordance therewith for the life of the project.
11. The proposed buildings and site elevations shall not exceed the heights identified in the aeronautical studies.
12. The maximum height and top point elevation specified above shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in structure height or elevation shall not require further review by the Airport Land Use Commission.
13. Temporary construction equipment used during actual construction of the structure(s) shall not exceed the structure heights and site elevations as identified in the aeronautical studies, unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.
14. Within five (5) days after construction of the proposed building reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned or a decision is made not to construct the applicable structure.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS

PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



Legend

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E

Boundary Lines

- Airport Property Line
- - - City Limits
- ⊛ Height Review Overlay Zone

Note

Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

See Chapter 2, Table 2A from compatibility criteria associated with this map.

**Riverside County
Airport Land Use Commission**

**Riverside County
Airport Land Use Compatibility Plan
Policy Document**

(April 2010)

Map FV-1

Compatibility Map
French Valley Airport



Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11049-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 1 - SE
 Location: Murrietta, CA
 Latitude: 33-34-23.64N NAD 83
 Longitude: 117-07-58.39W
 Heights: 1328 feet site elevation (SE)
 50 feet above ground level (AGL)
 1378 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11049-OE.

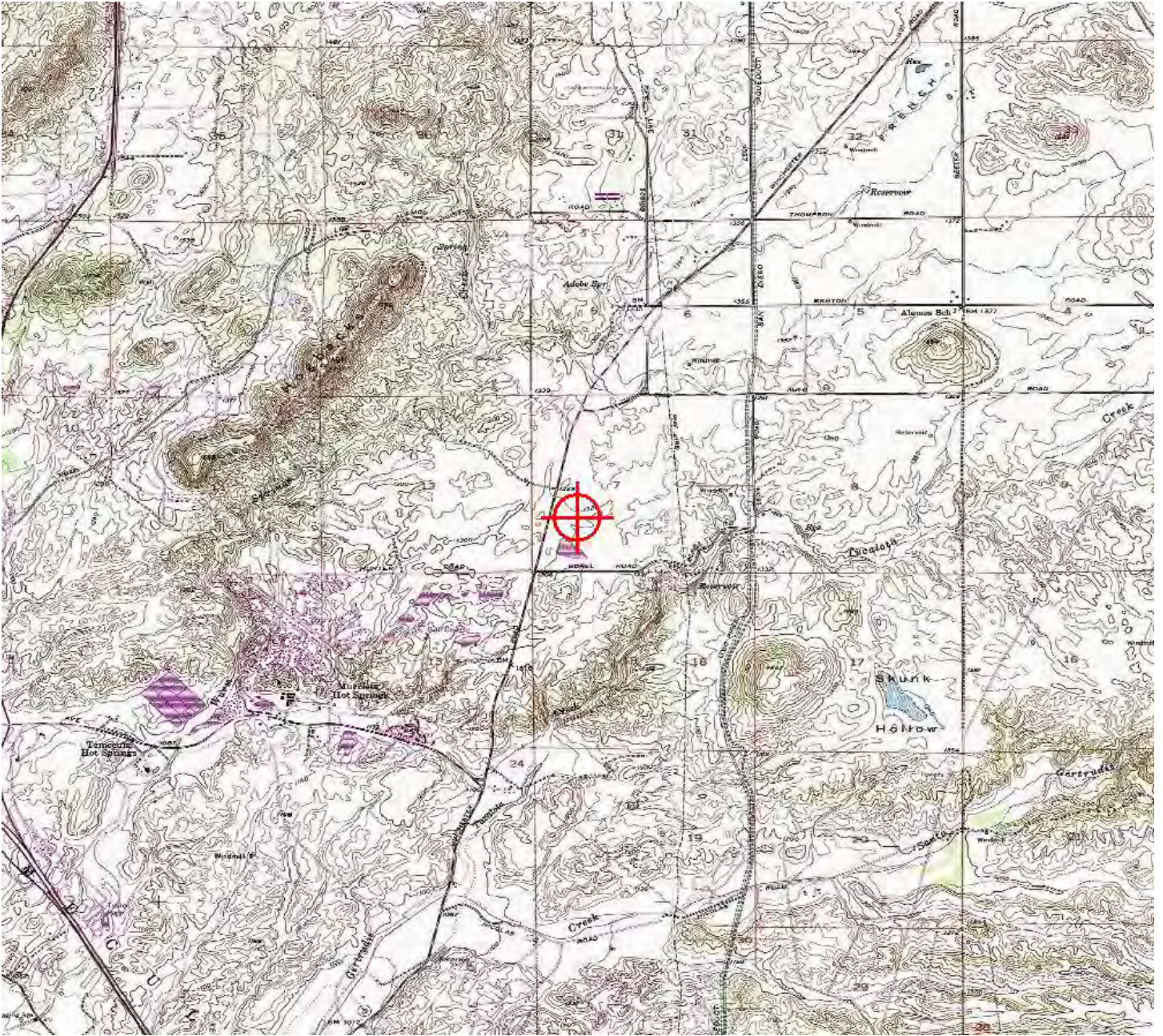
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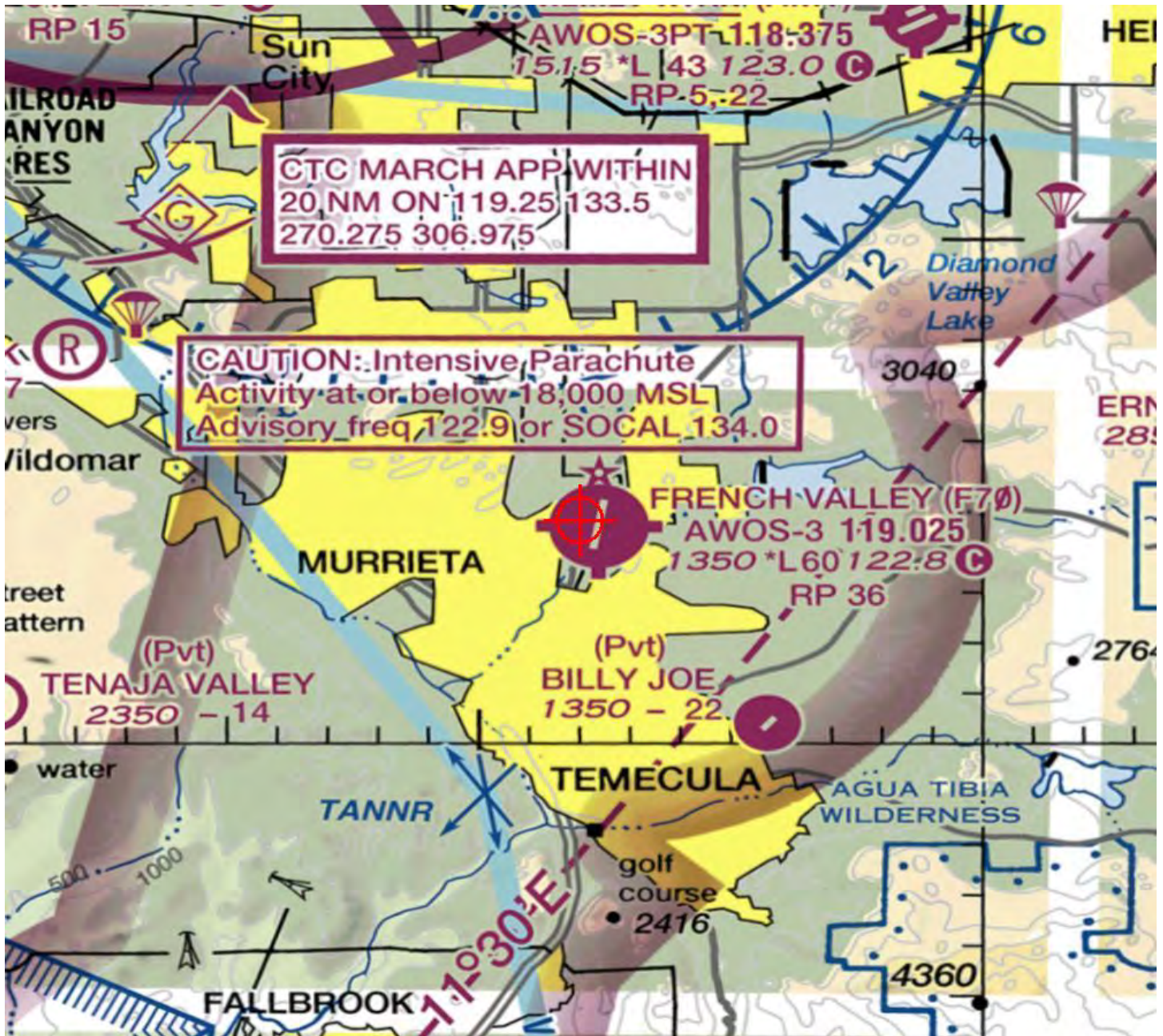
(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-11049-OE







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11050-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 1 - SW
 Location: Murrietta, CA
 Latitude: 33-34-23.88N NAD 83
 Longitude: 117-07-59.70W
 Heights: 1330 feet site elevation (SE)
 50 feet above ground level (AGL)
 1380 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

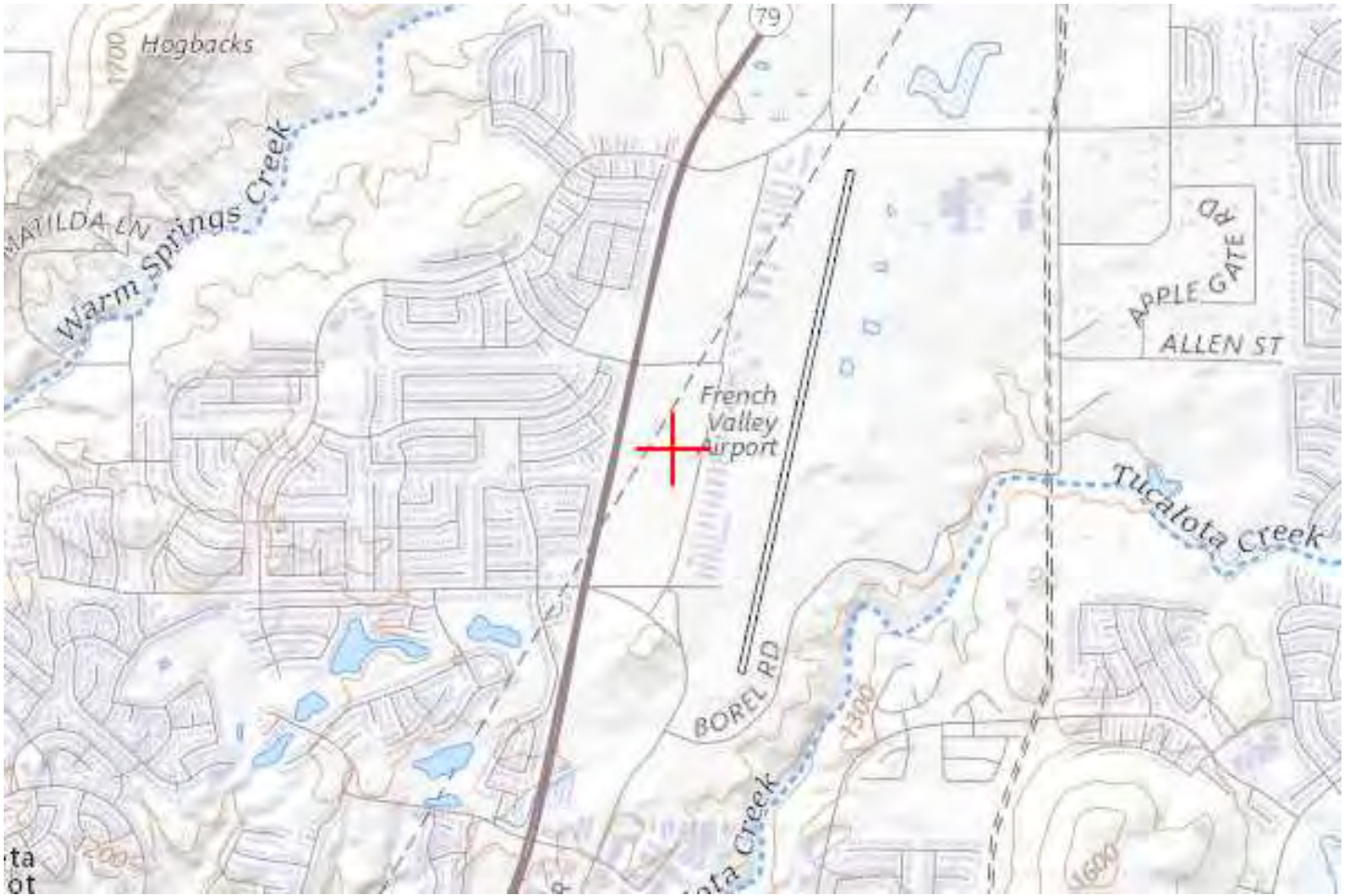
If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11050-OE.

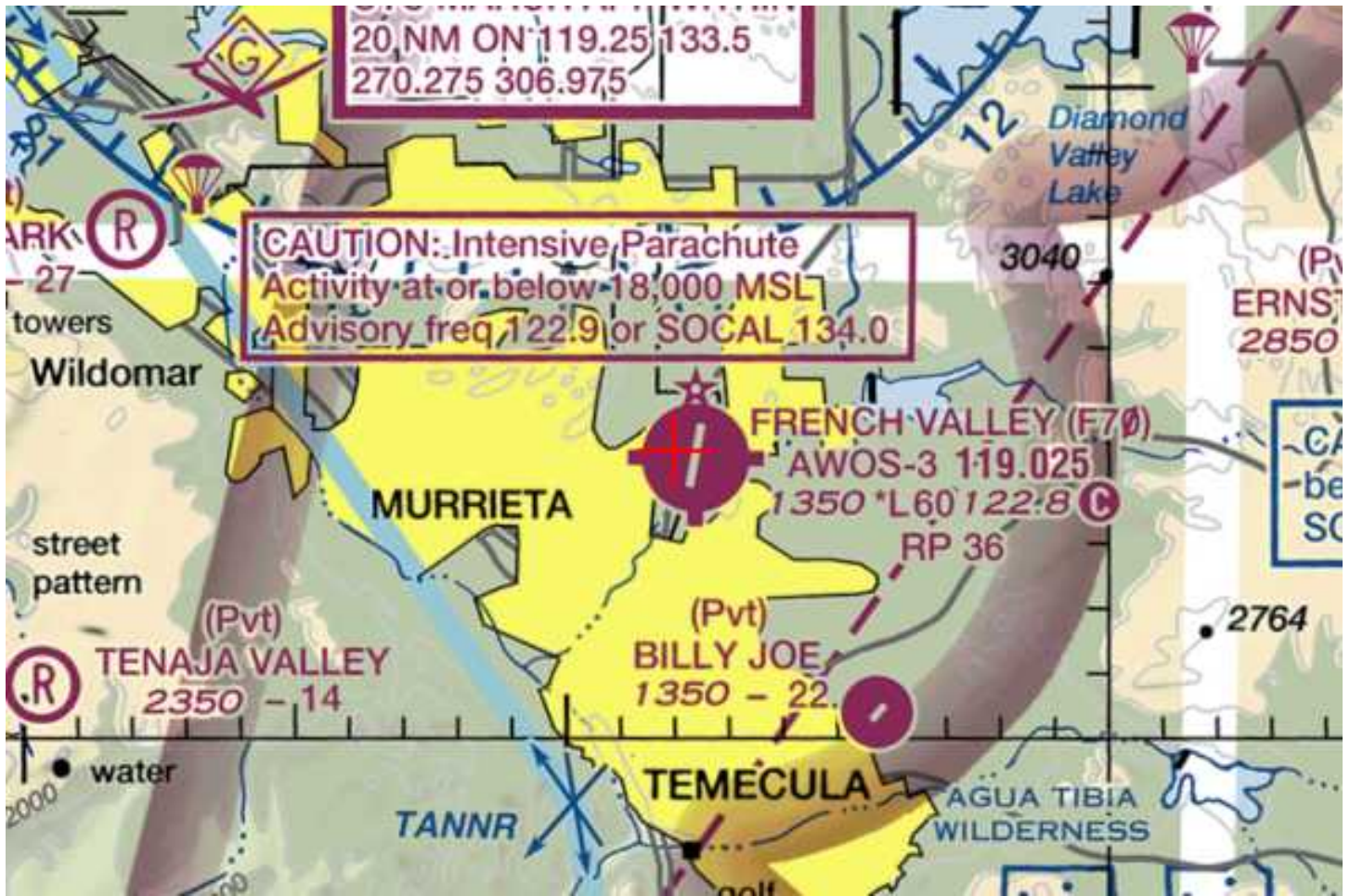
Signature Control No: 592524657-594010604

(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11051-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 1 - NE
 Location: Murrietta, CA
 Latitude: 33-34-26.70N NAD 83
 Longitude: 117-07-56.45W
 Heights: 1328 feet site elevation (SE)
 50 feet above ground level (AGL)
 1378 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

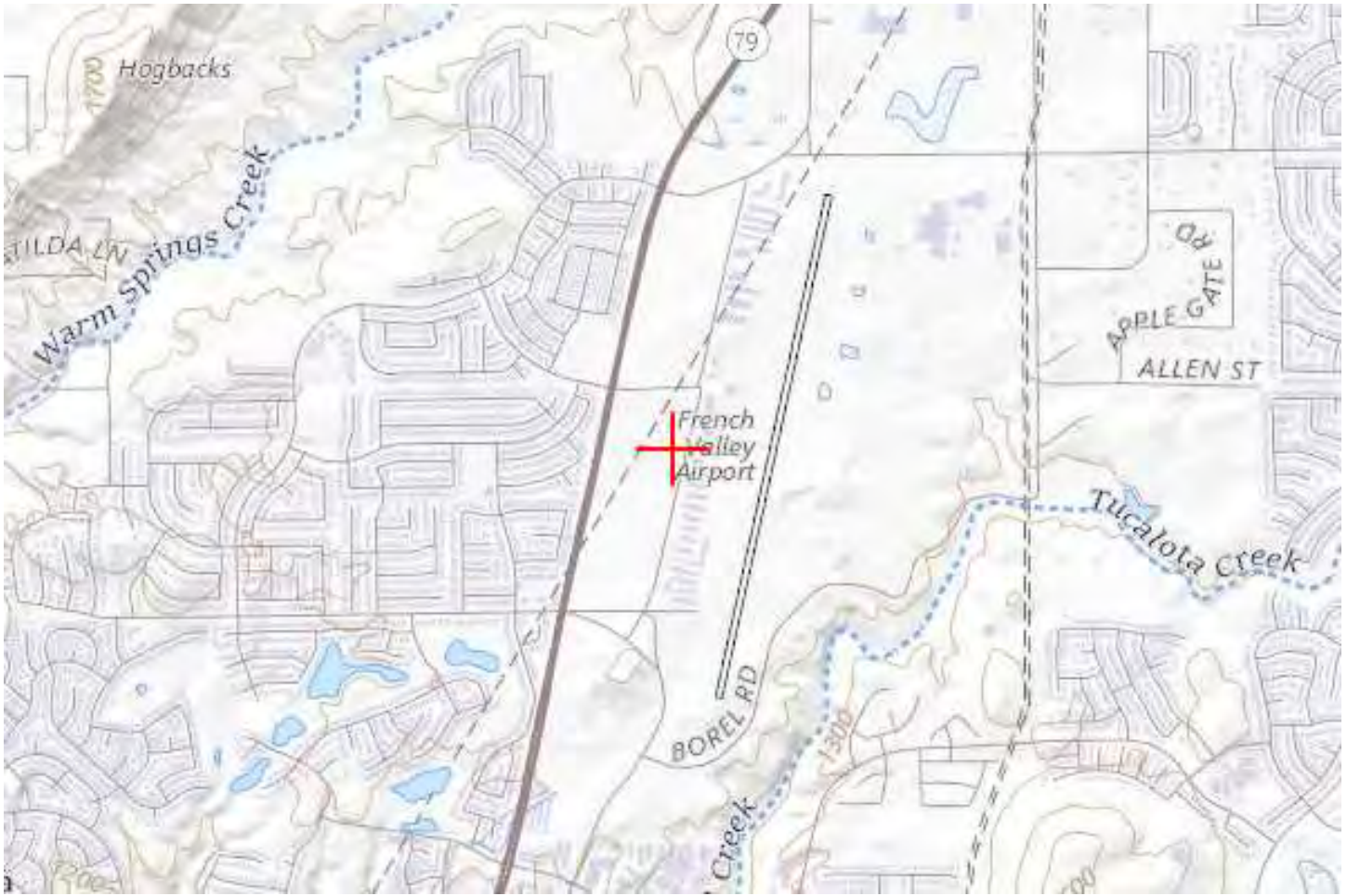
If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11051-OE.

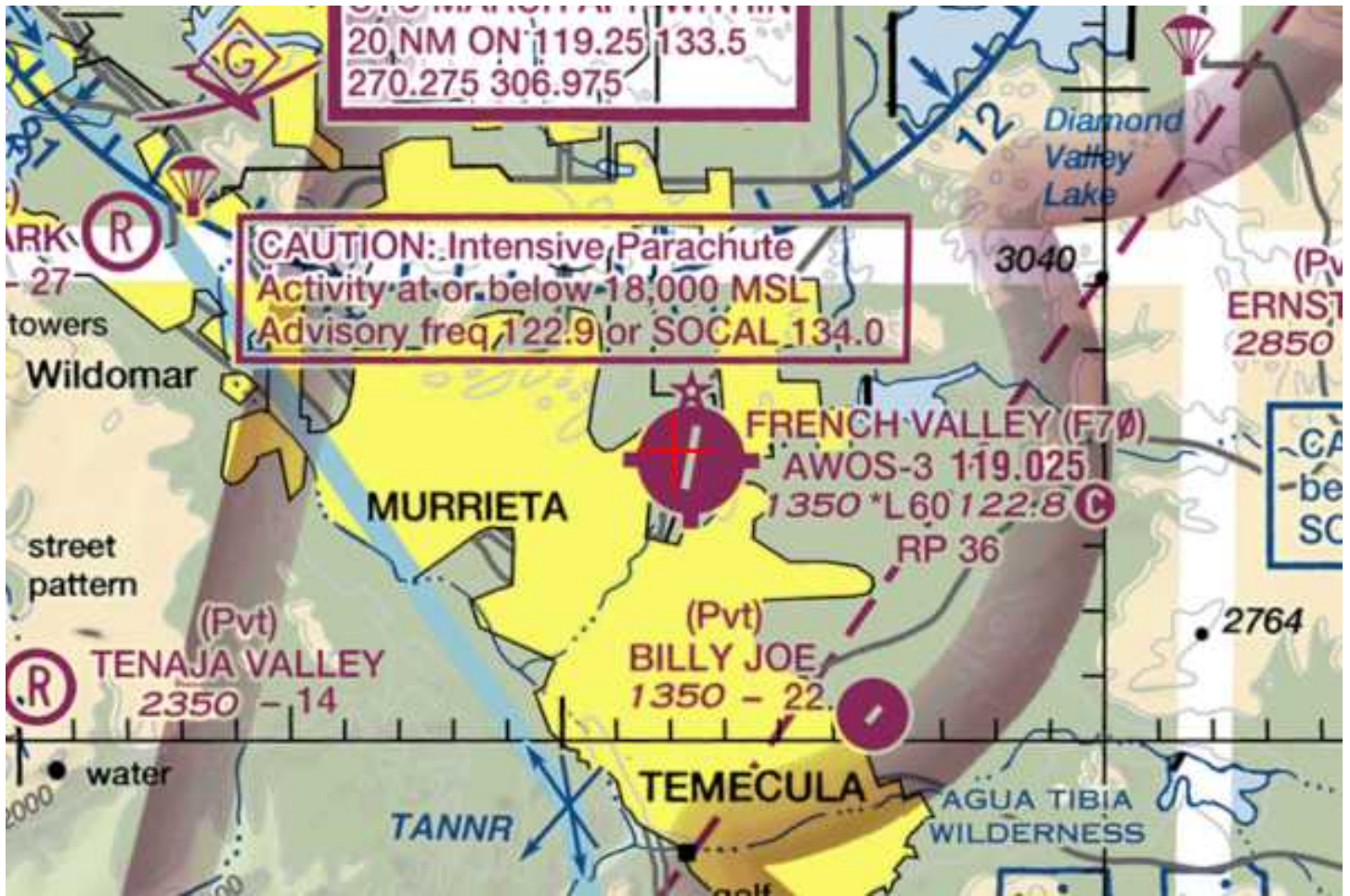
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(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11052-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 1 - NW
 Location: Murrietta, CA
 Latitude: 33-34-27.14N NAD 83
 Longitude: 117-07-58.83W
 Heights: 1331 feet site elevation (SE)
 50 feet above ground level (AGL)
 1381 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

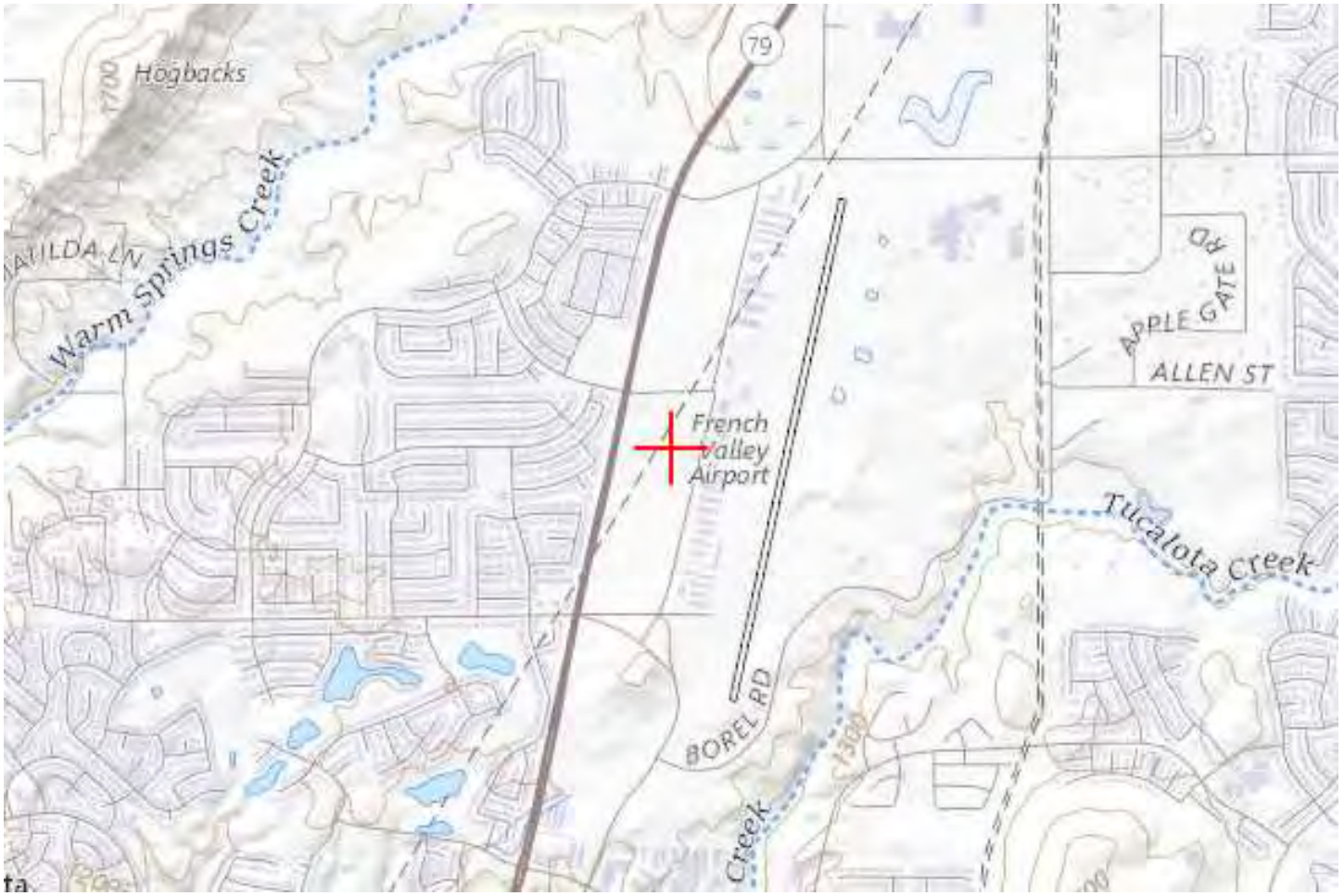
If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11052-OE.

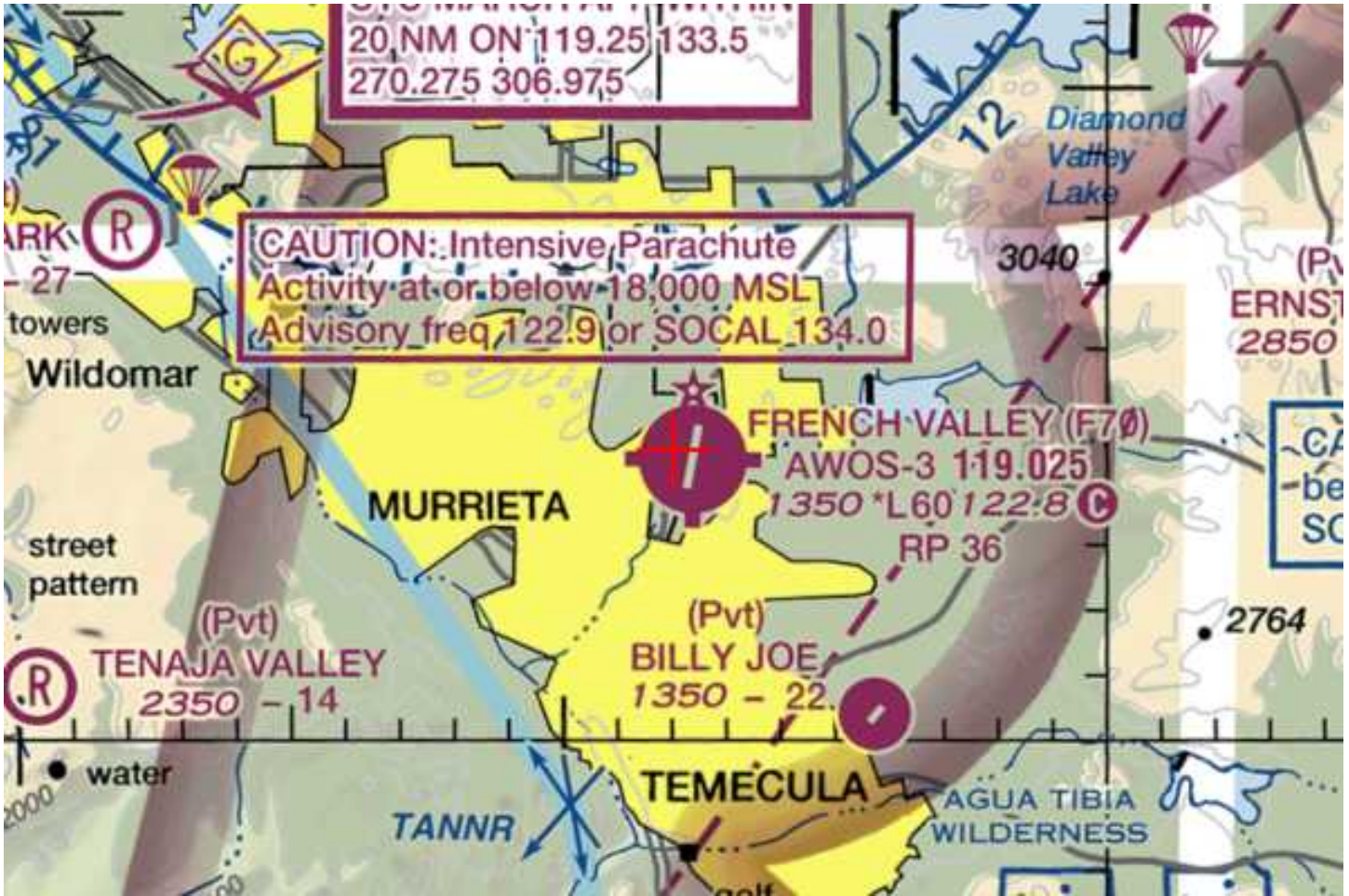
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(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11053-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 2 - SE
 Location: Murrietta, CA
 Latitude: 33-34-23.04N NAD 83
 Longitude: 117-08-01.19W
 Heights: 1324 feet site elevation (SE)
 50 feet above ground level (AGL)
 1374 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
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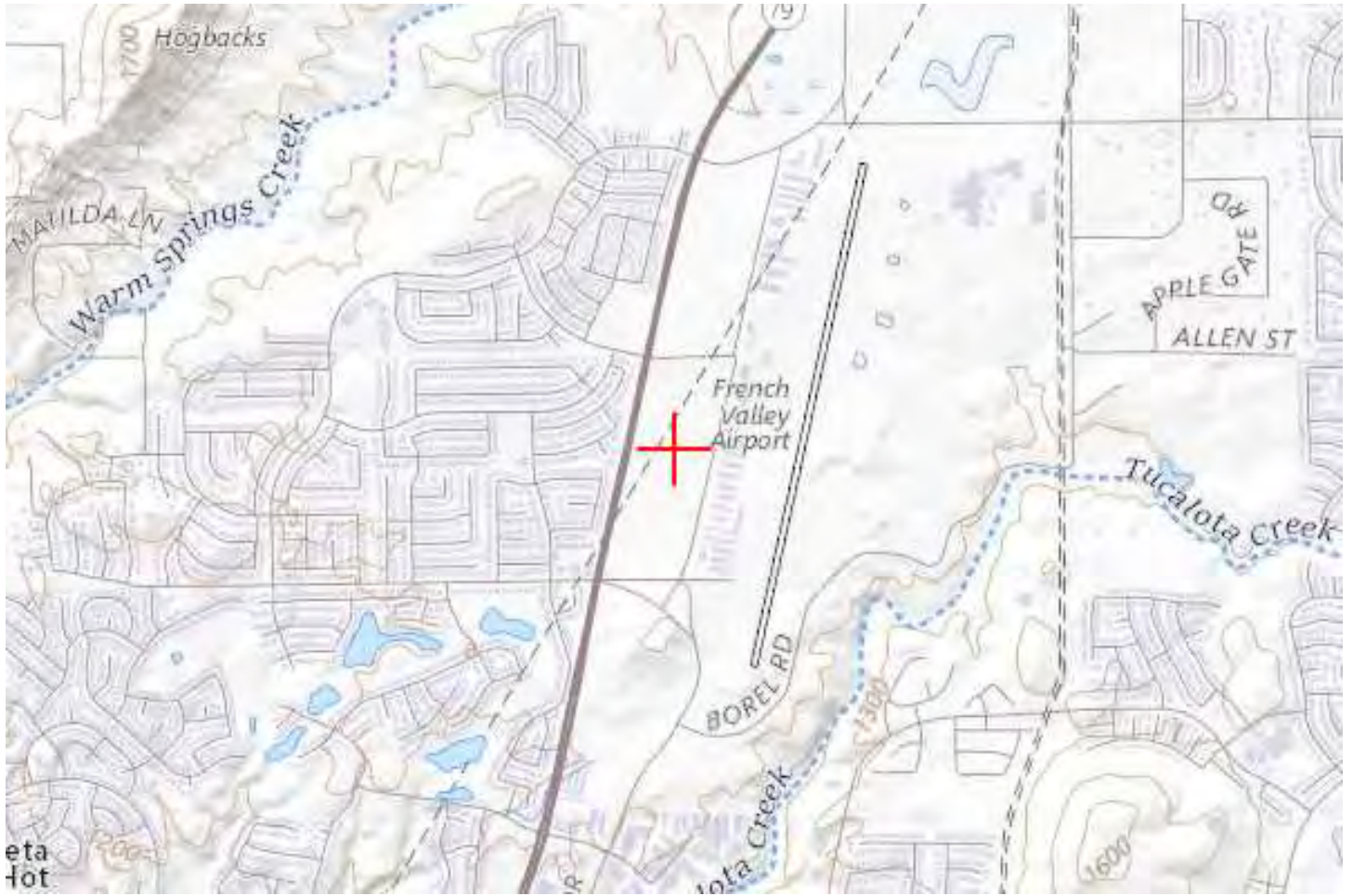
If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11053-OE.

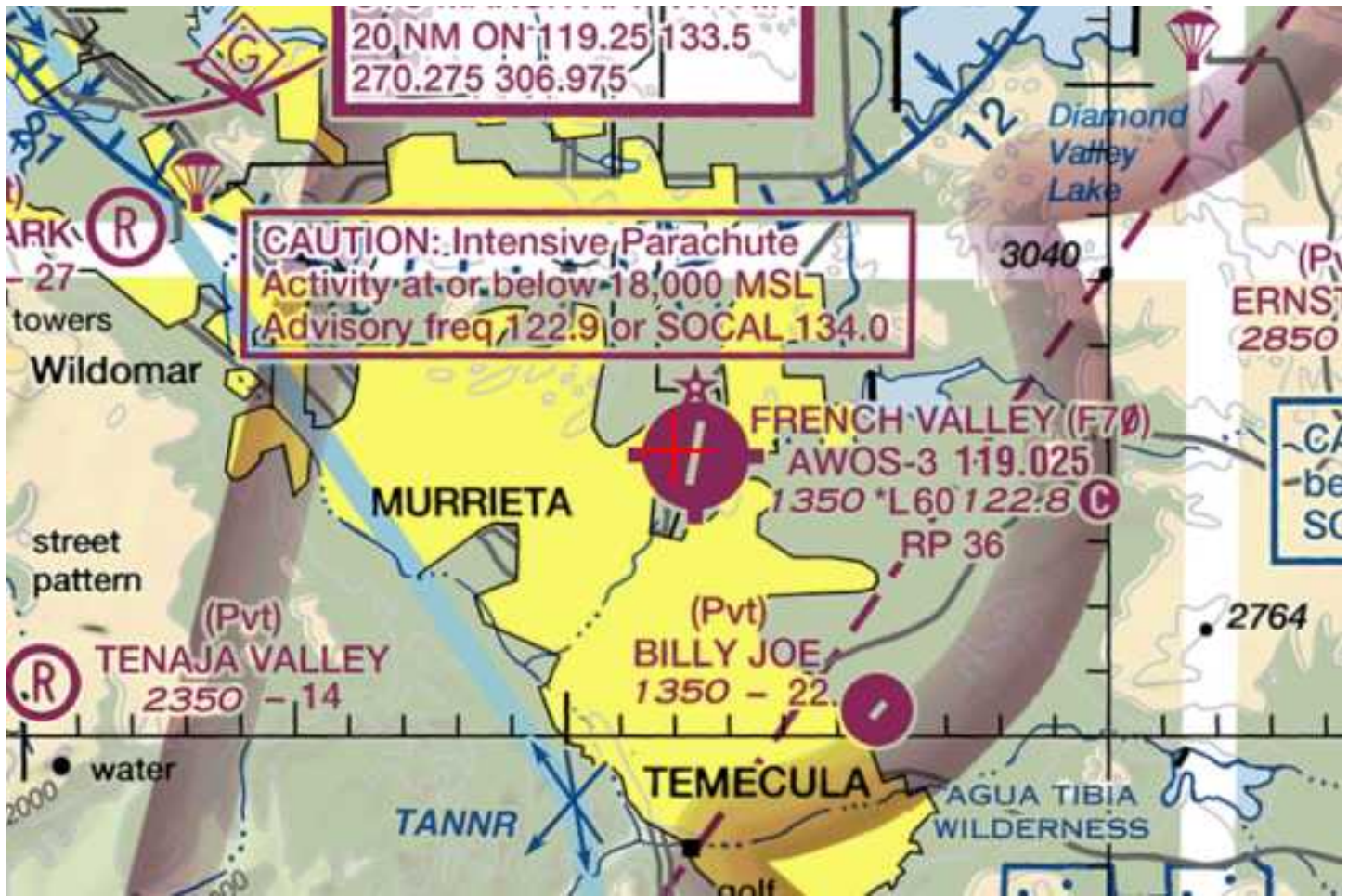
Signature Control No: 592524662-594010611

(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11054-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 2 - SW
 Location: Murrietta, CA
 Latitude: 33-34-23.43N NAD 83
 Longitude: 117-08-03.33W
 Heights: 1330 feet site elevation (SE)
 50 feet above ground level (AGL)
 1380 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

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The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

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- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

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If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11054-OE.

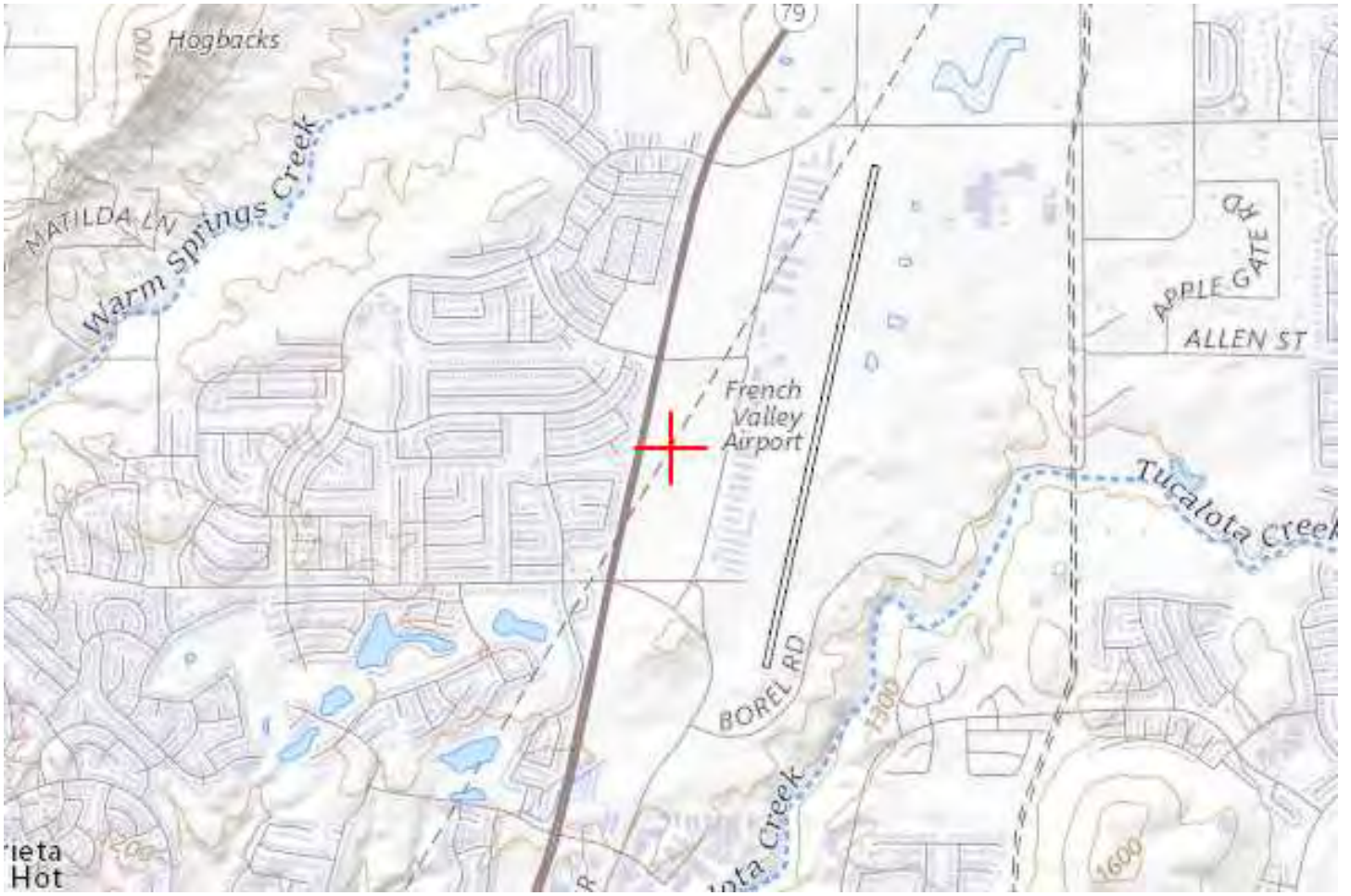
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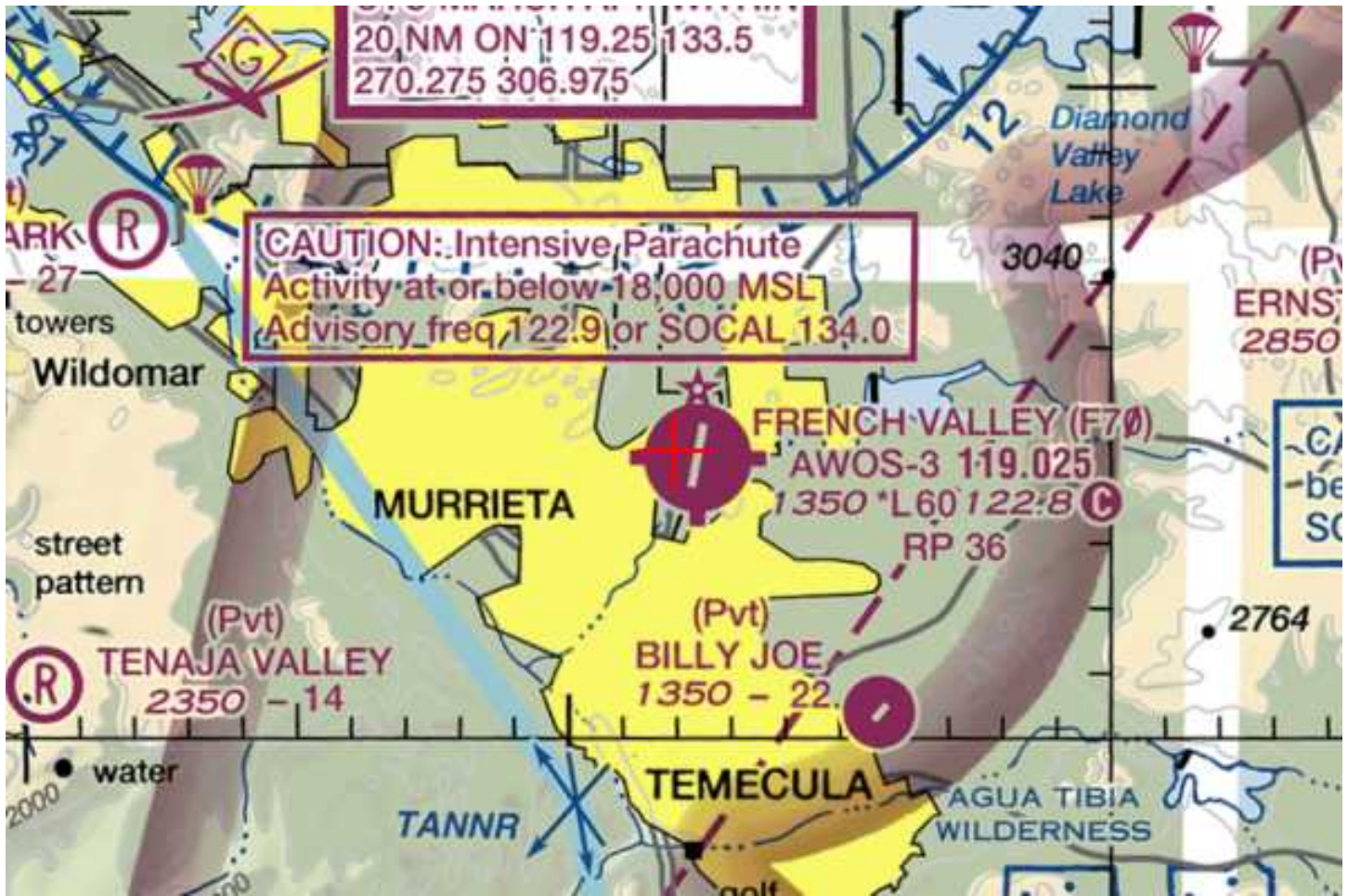
(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-11054-OE







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11055-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 2 - NE
 Location: Murrietta, CA
 Latitude: 33-34-27.36N NAD 83
 Longitude: 117-07-59.85W
 Heights: 1333 feet site elevation (SE)
 50 feet above ground level (AGL)
 1383 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

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NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

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If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11055-OE.

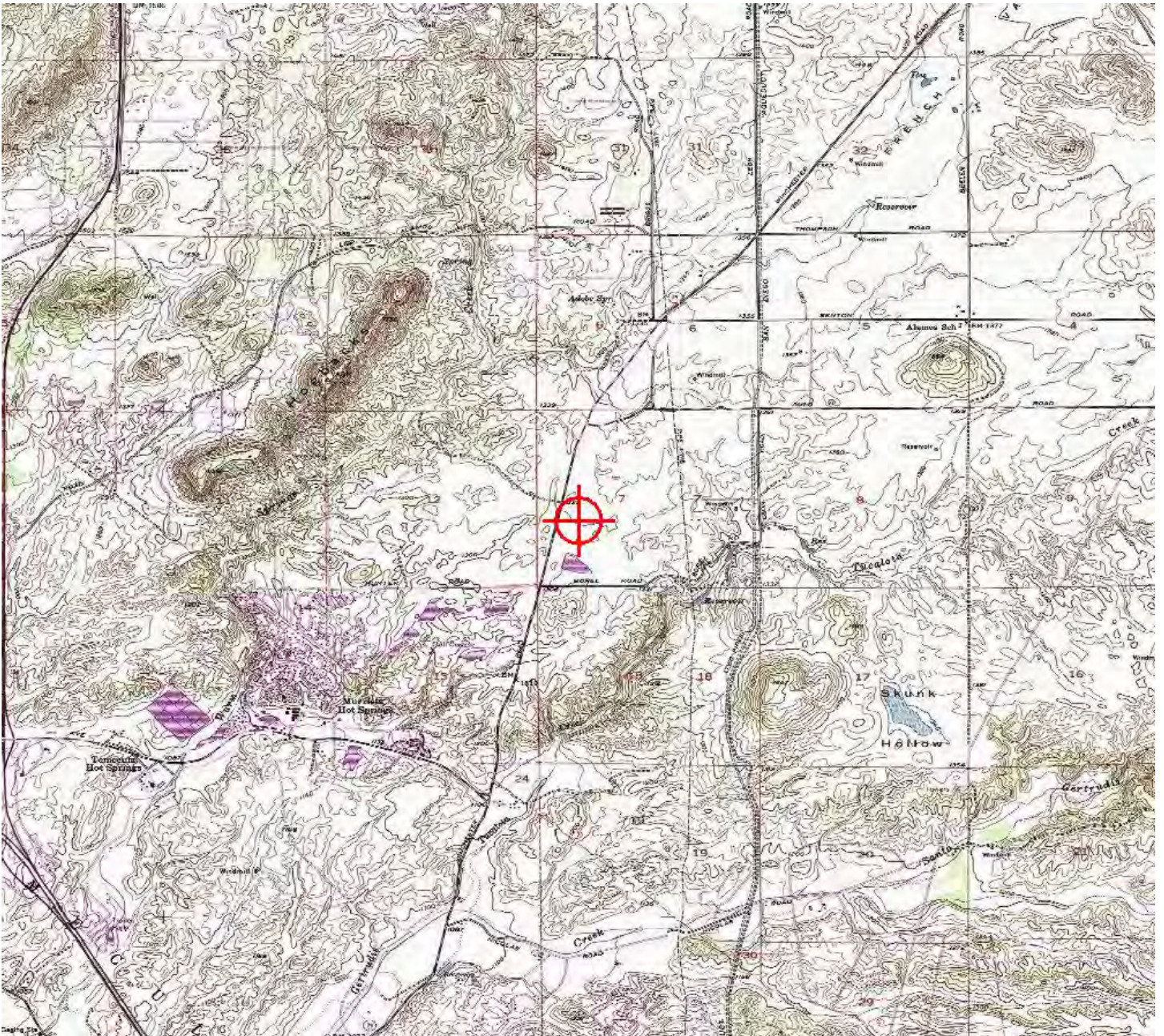
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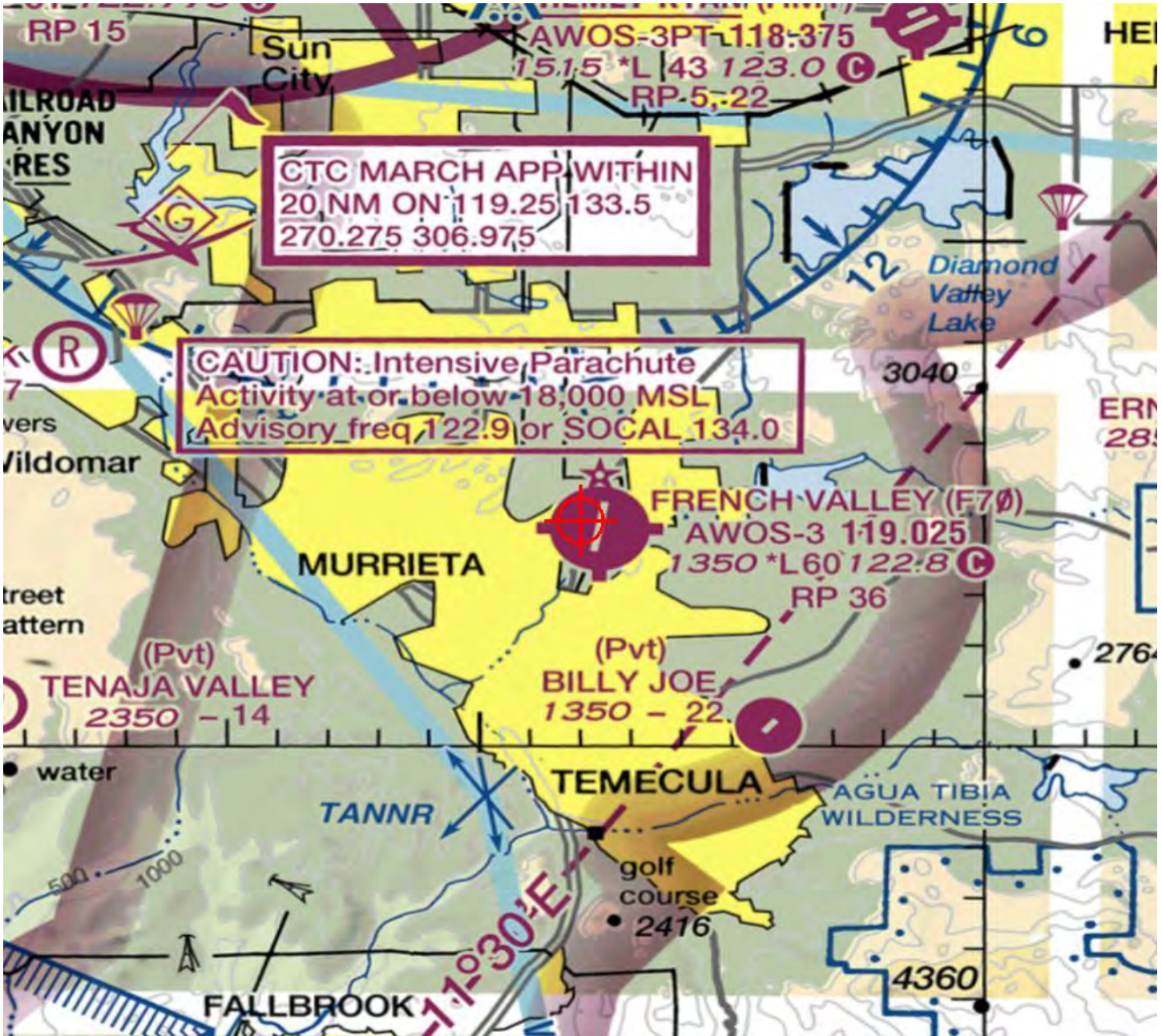
(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-11055-OE







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11056-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 2 - NW
 Location: Murrietta, CA
 Latitude: 33-34-27.75N NAD 83
 Longitude: 117-08-02.22W
 Heights: 1331 feet site elevation (SE)
 50 feet above ground level (AGL)
 1381 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11056-OE.

Signature Control No: 592524665-594010607

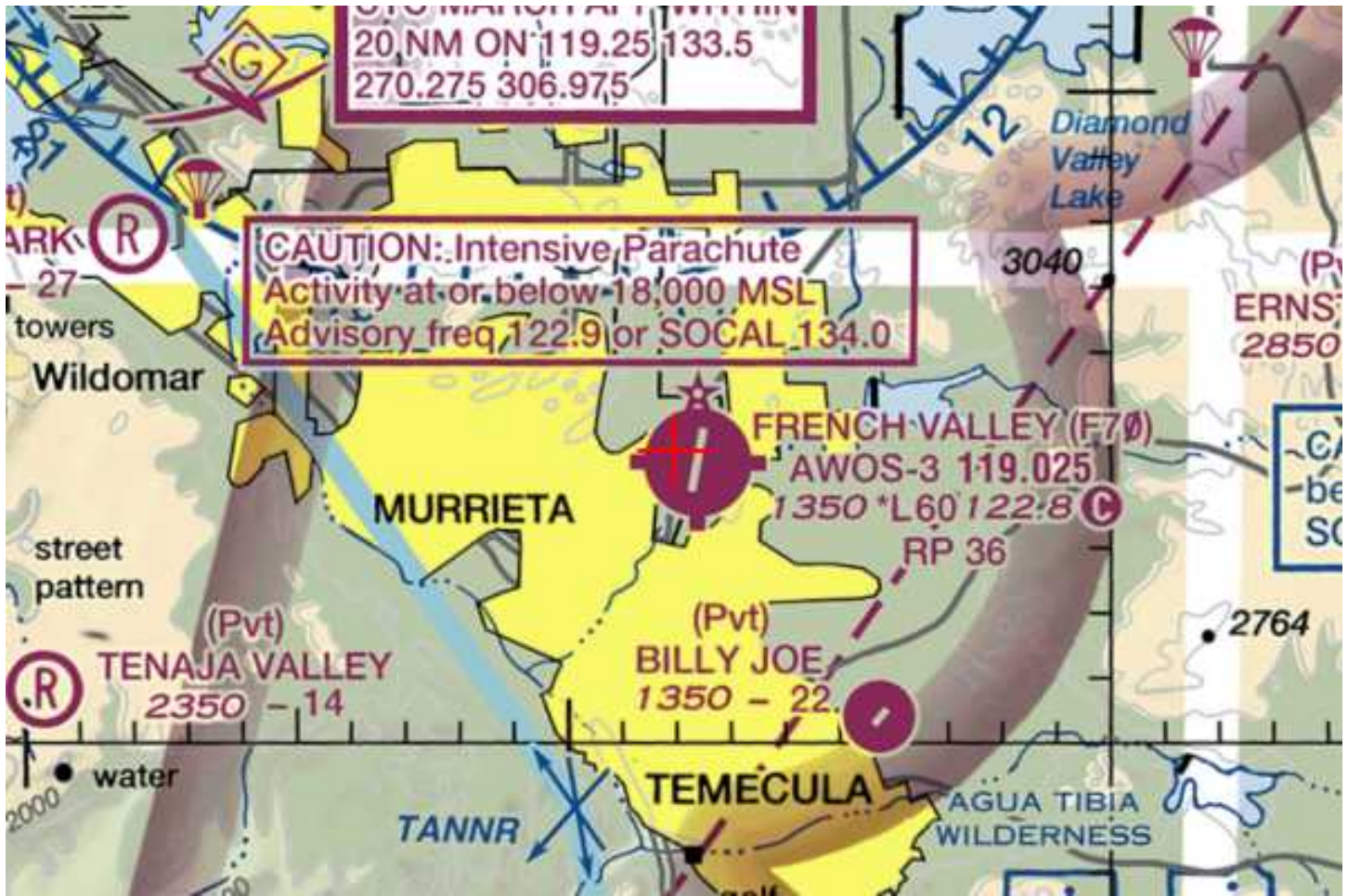
(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-11056-OE







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11057-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 3 - SE
 Location: Murrietta, CA
 Latitude: 33-34-28.26N NAD 83
 Longitude: 117-07-59.55W
 Heights: 1329 feet site elevation (SE)
 50 feet above ground level (AGL)
 1379 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

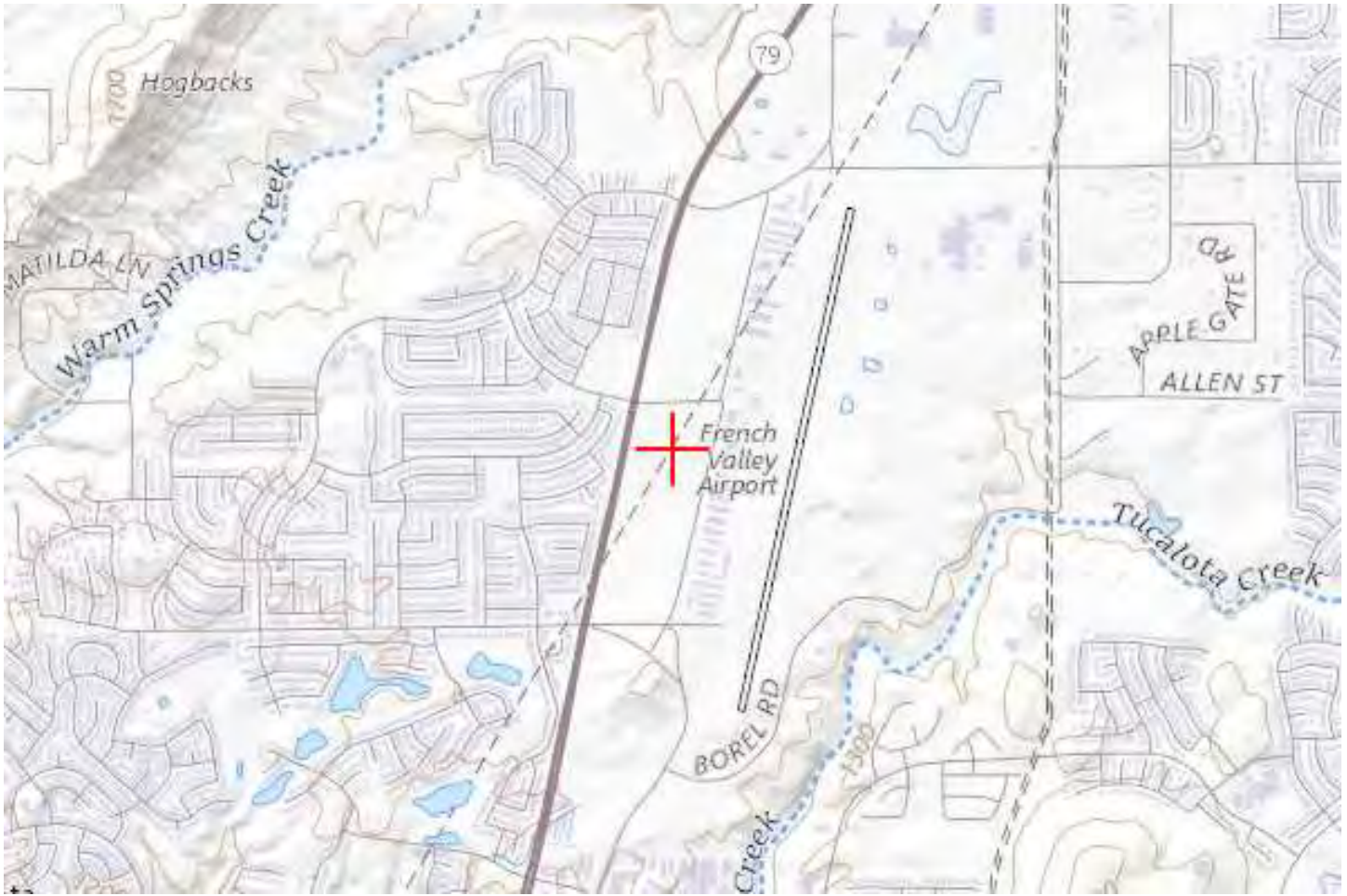
If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11057-OE.

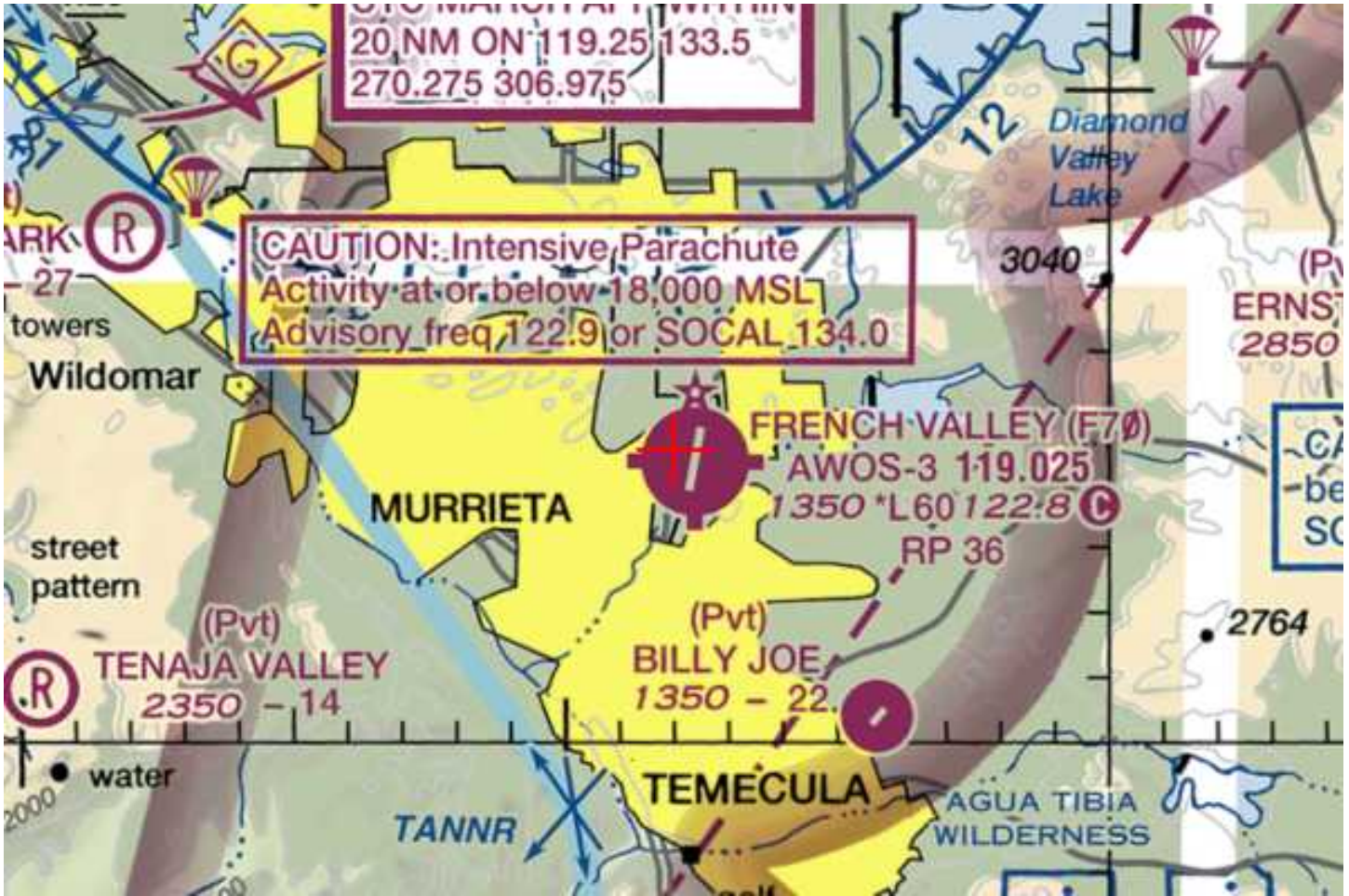
Signature Control No: 592524666-594010615

(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11058-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 3 - SW
 Location: Murrietta, CA
 Latitude: 33-34-28.71N NAD 83
 Longitude: 117-08-01.92W
 Heights: 1330 feet site elevation (SE)
 50 feet above ground level (AGL)
 1380 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

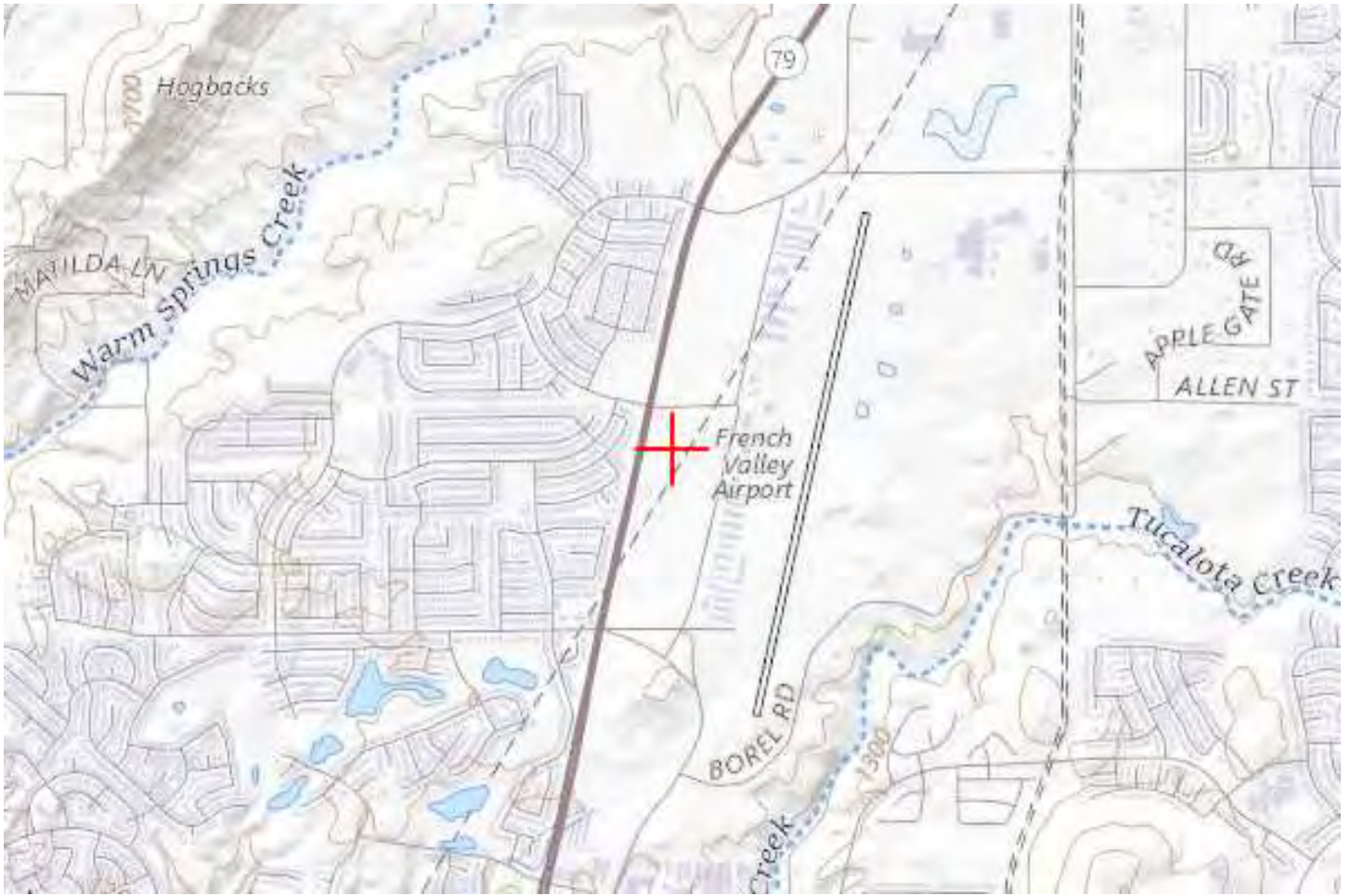
If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11058-OE.

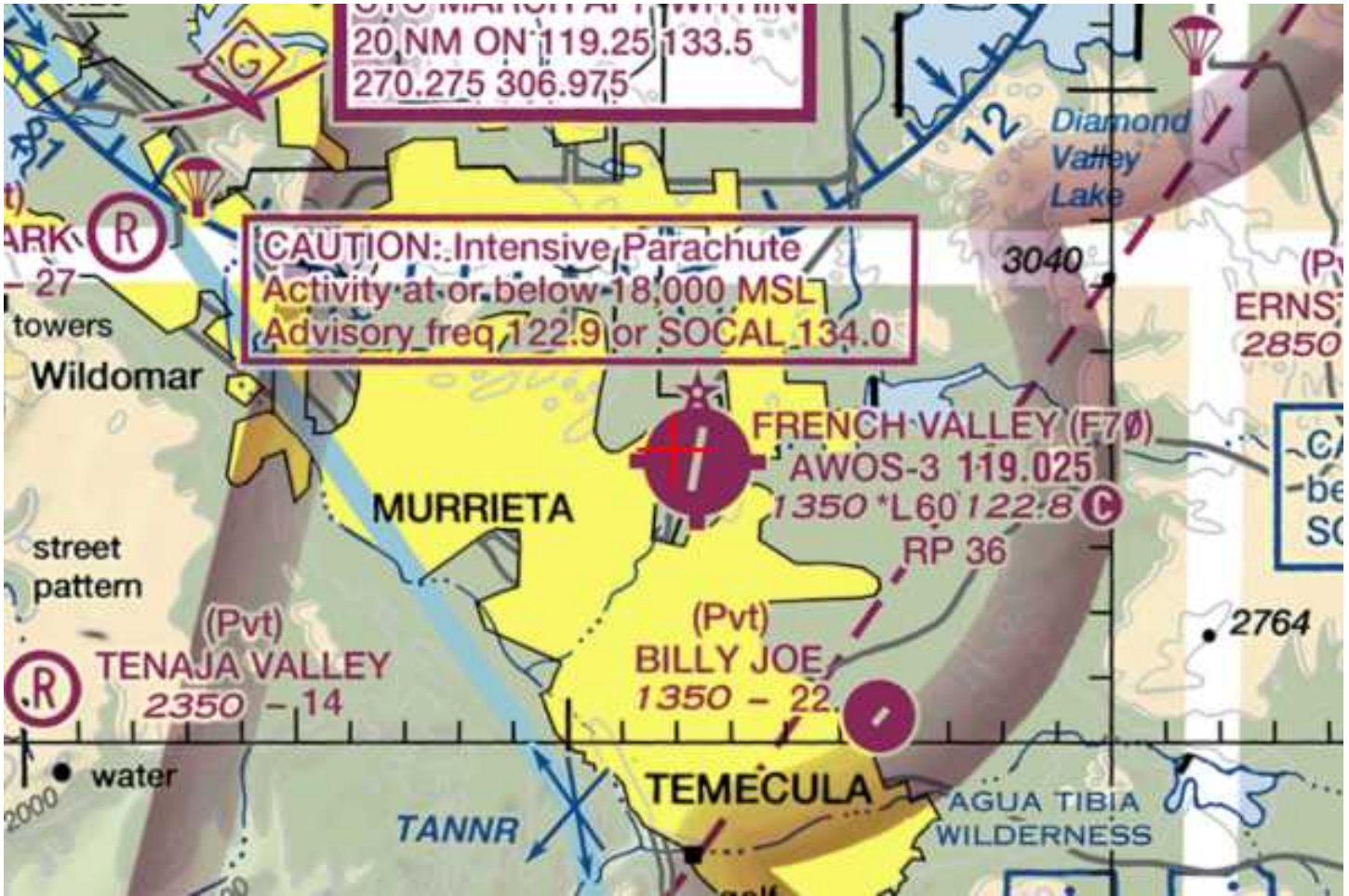
Signature Control No: 592524667-594010620

(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11059-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 3 - NE
 Location: Murrietta, CA
 Latitude: 33-34-32.52N NAD 83
 Longitude: 117-07-58.50W
 Heights: 1331 feet site elevation (SE)
 50 feet above ground level (AGL)
 1381 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11059-OE.

Signature Control No: 592524668-594010609

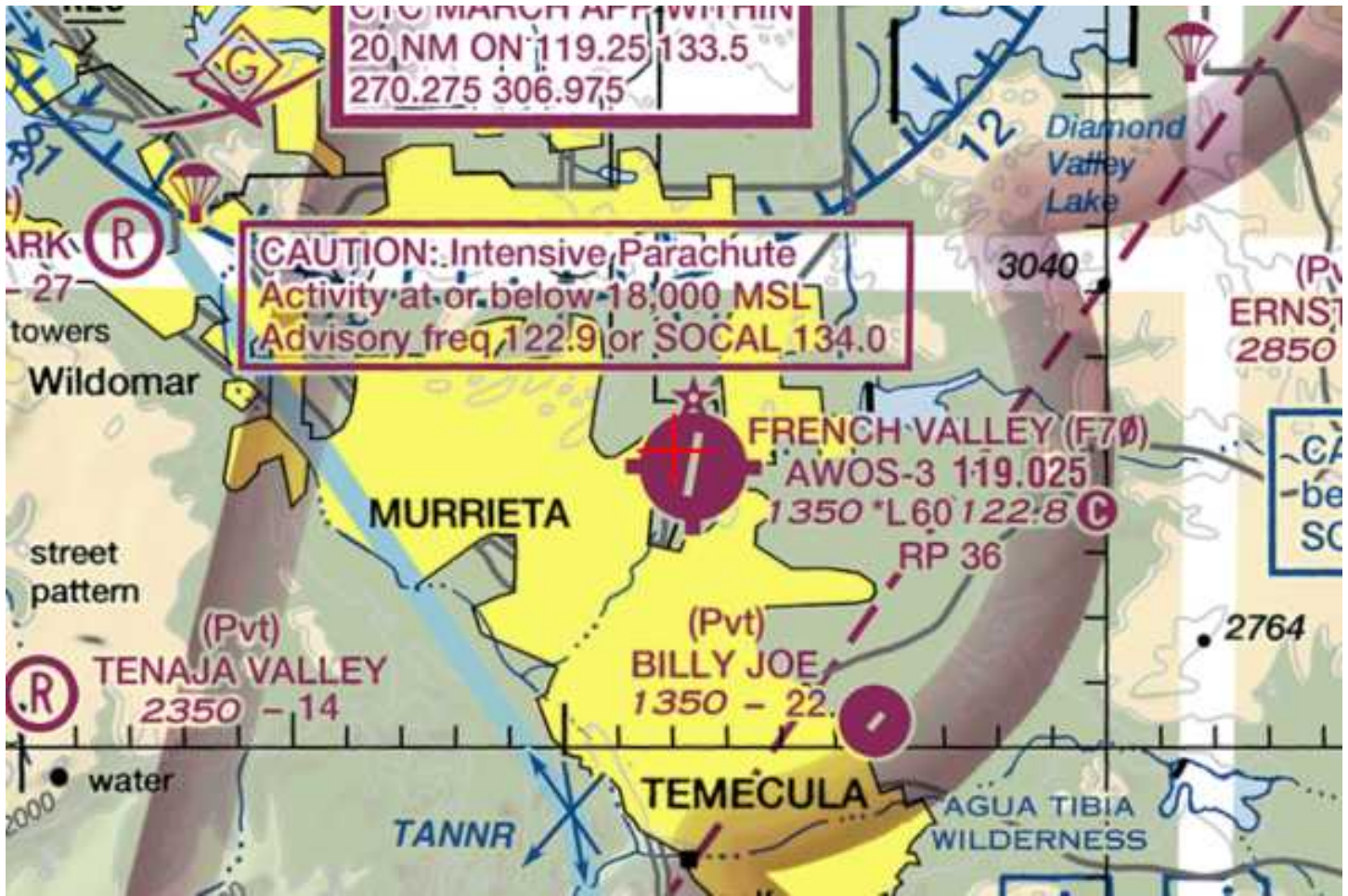
(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-11059-OE







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11060-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 3 - NW
 Location: Murrietta, CA
 Latitude: 33-34-32.80N NAD 83
 Longitude: 117-08-00.05W
 Heights: 1329 feet site elevation (SE)
 50 feet above ground level (AGL)
 1379 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11060-OE.

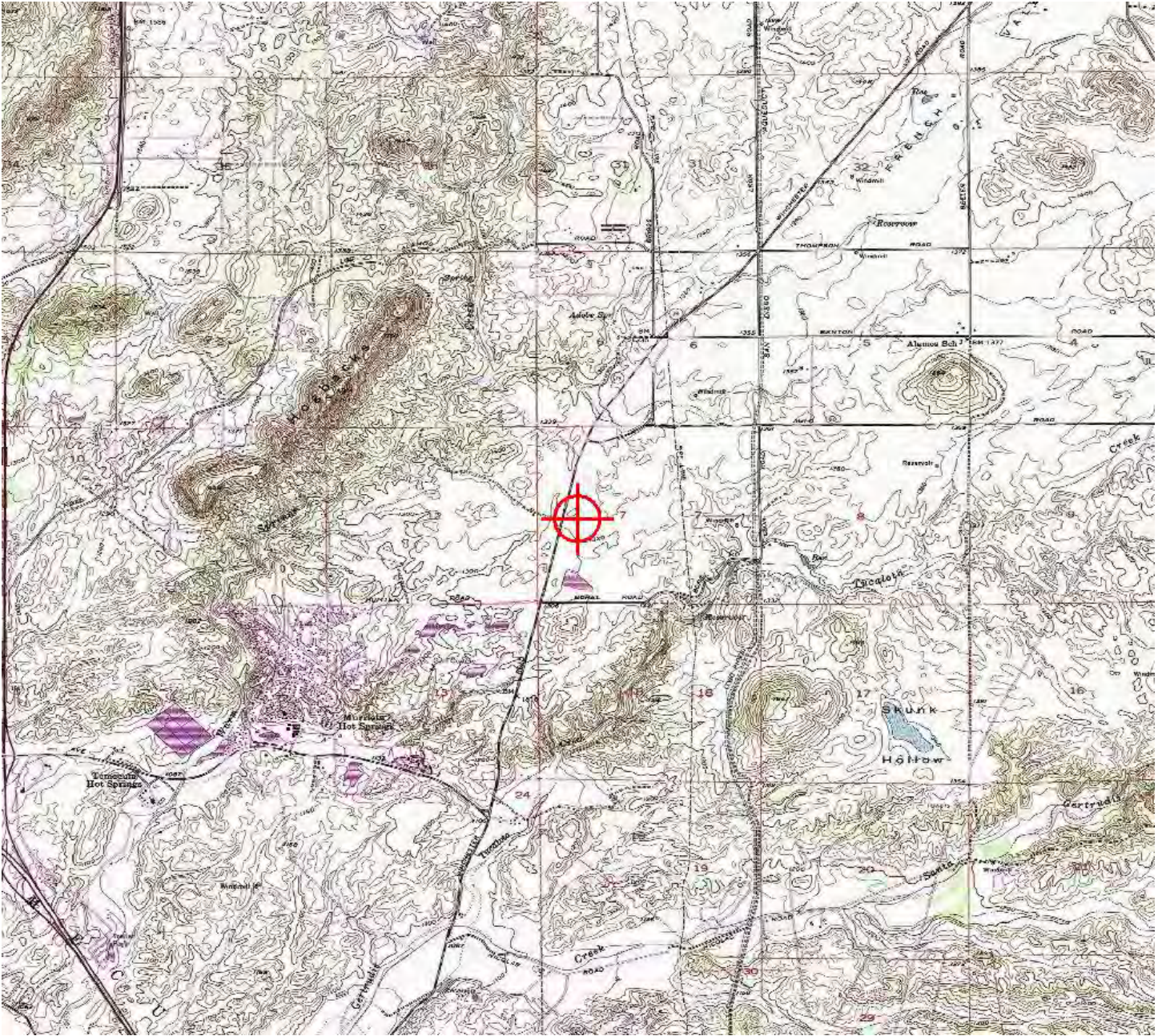
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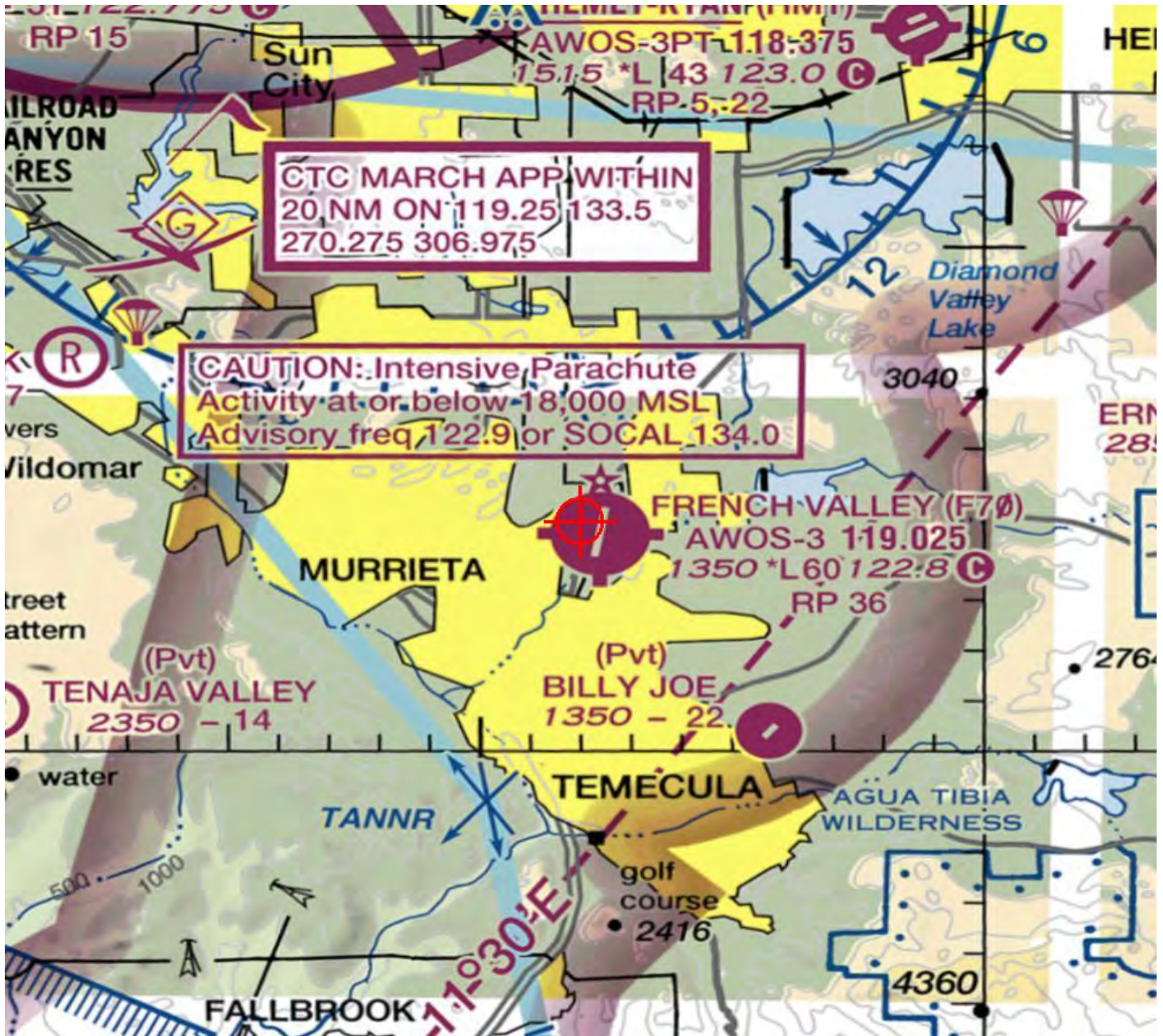
(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-11060-OE







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11061-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 4 - SE
 Location: Murrietta, CA
 Latitude: 33-34-27.69N NAD 83
 Longitude: 117-07-56.18W
 Heights: 1330 feet site elevation (SE)
 50 feet above ground level (AGL)
 1380 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

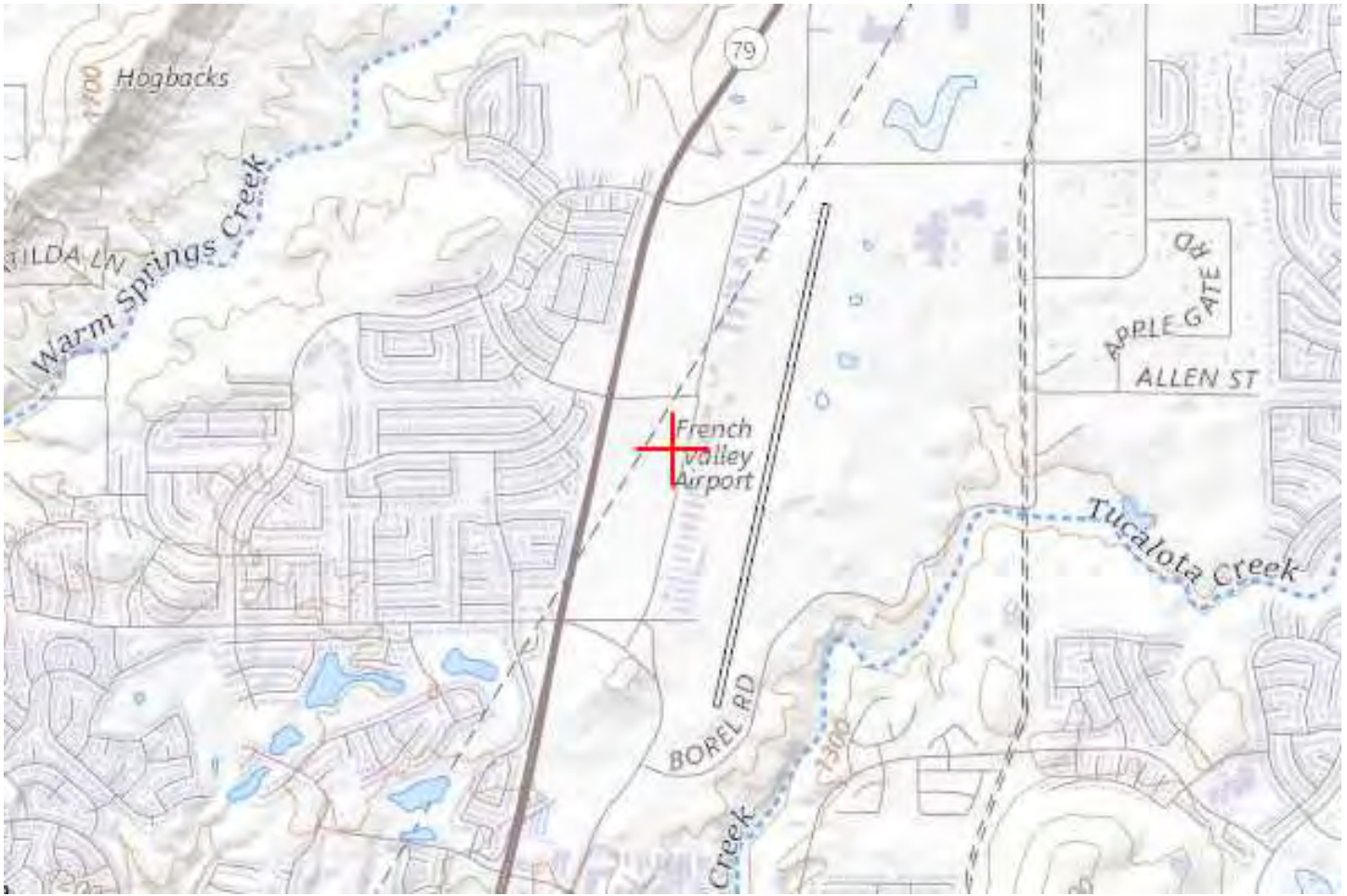
If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11061-OE.

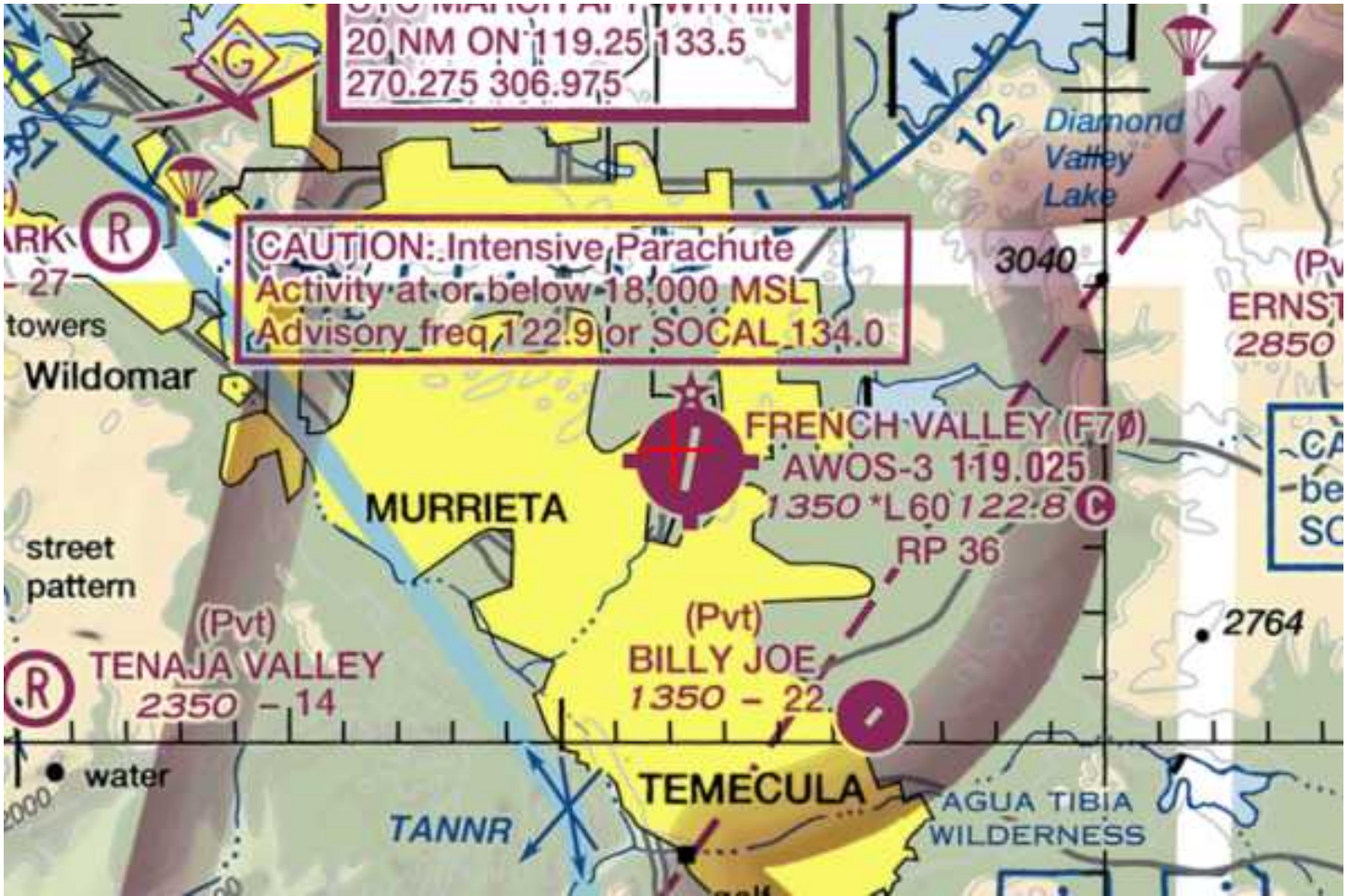
Signature Control No: 592524670-594010618

(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11062-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 4 - SW
 Location: Murrietta, CA
 Latitude: 33-34-28.10N NAD 83
 Longitude: 117-07-58.62W
 Heights: 1334 feet site elevation (SE)
 50 feet above ground level (AGL)
 1384 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11062-OE.

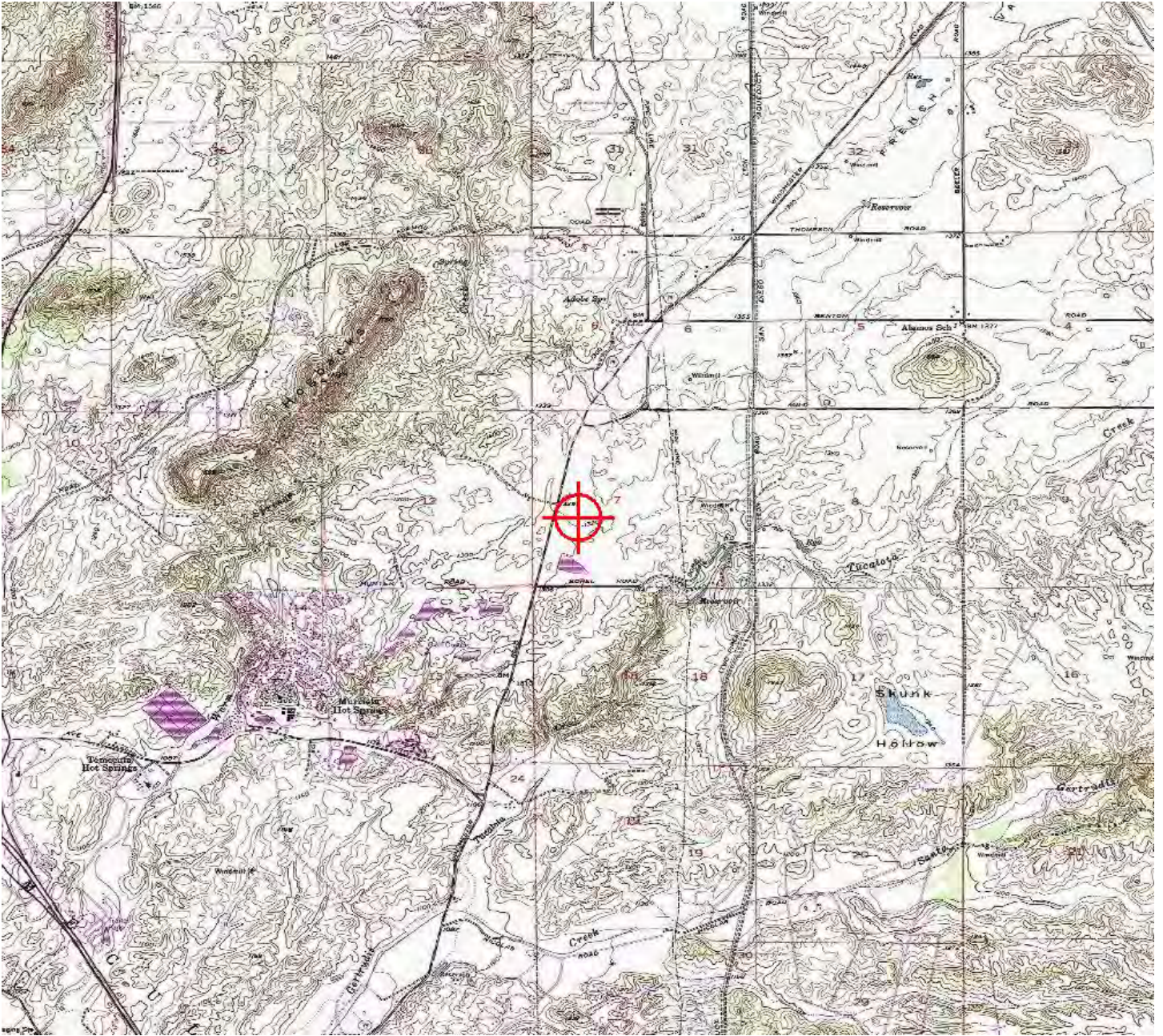
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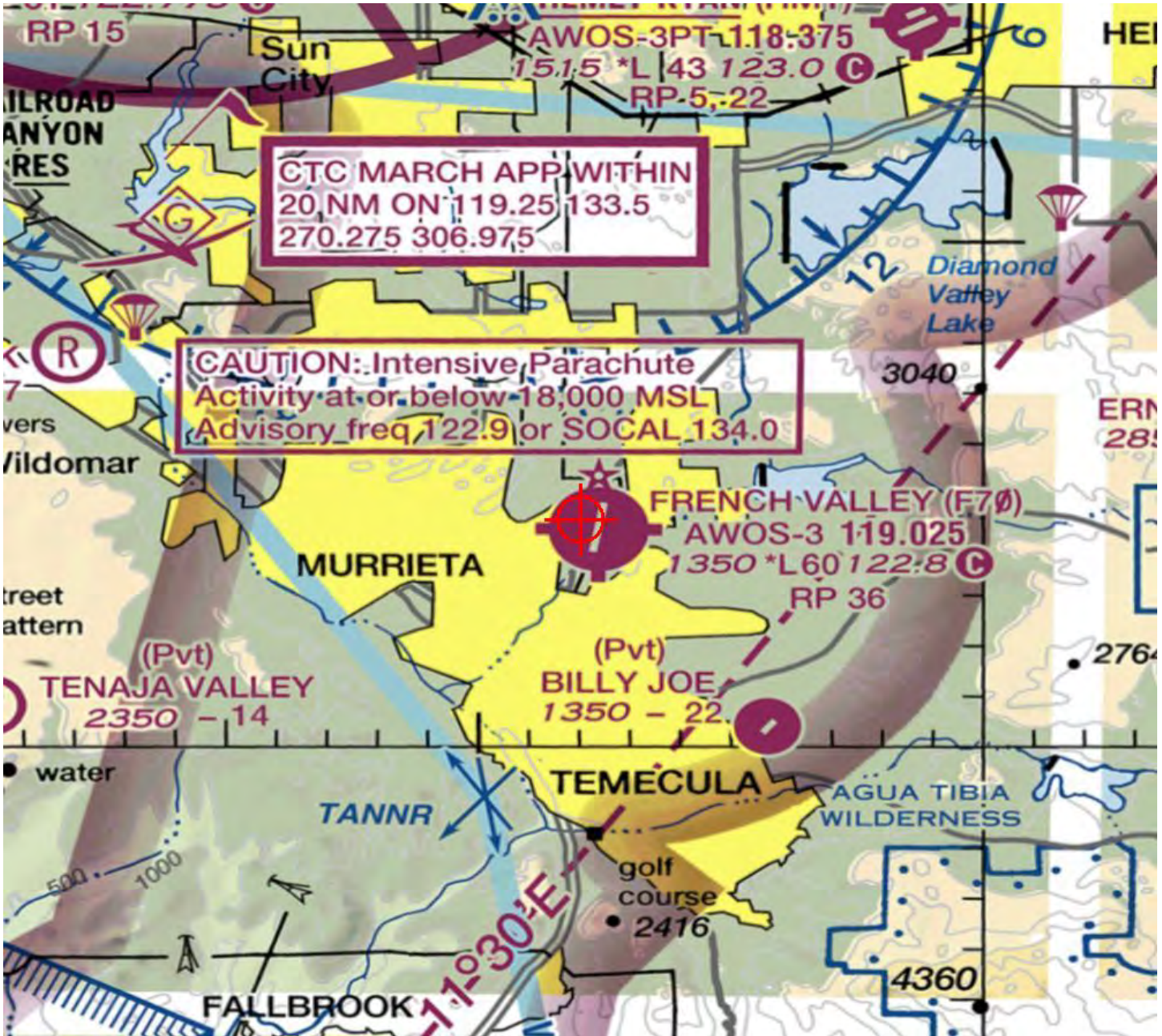
(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-11062-OE







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11063-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 4 - NE
 Location: Murrietta, CA
 Latitude: 33-34-31.89N NAD 83
 Longitude: 117-07-55.05W
 Heights: 1332 feet site elevation (SE)
 50 feet above ground level (AGL)
 1382 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11063-OE.

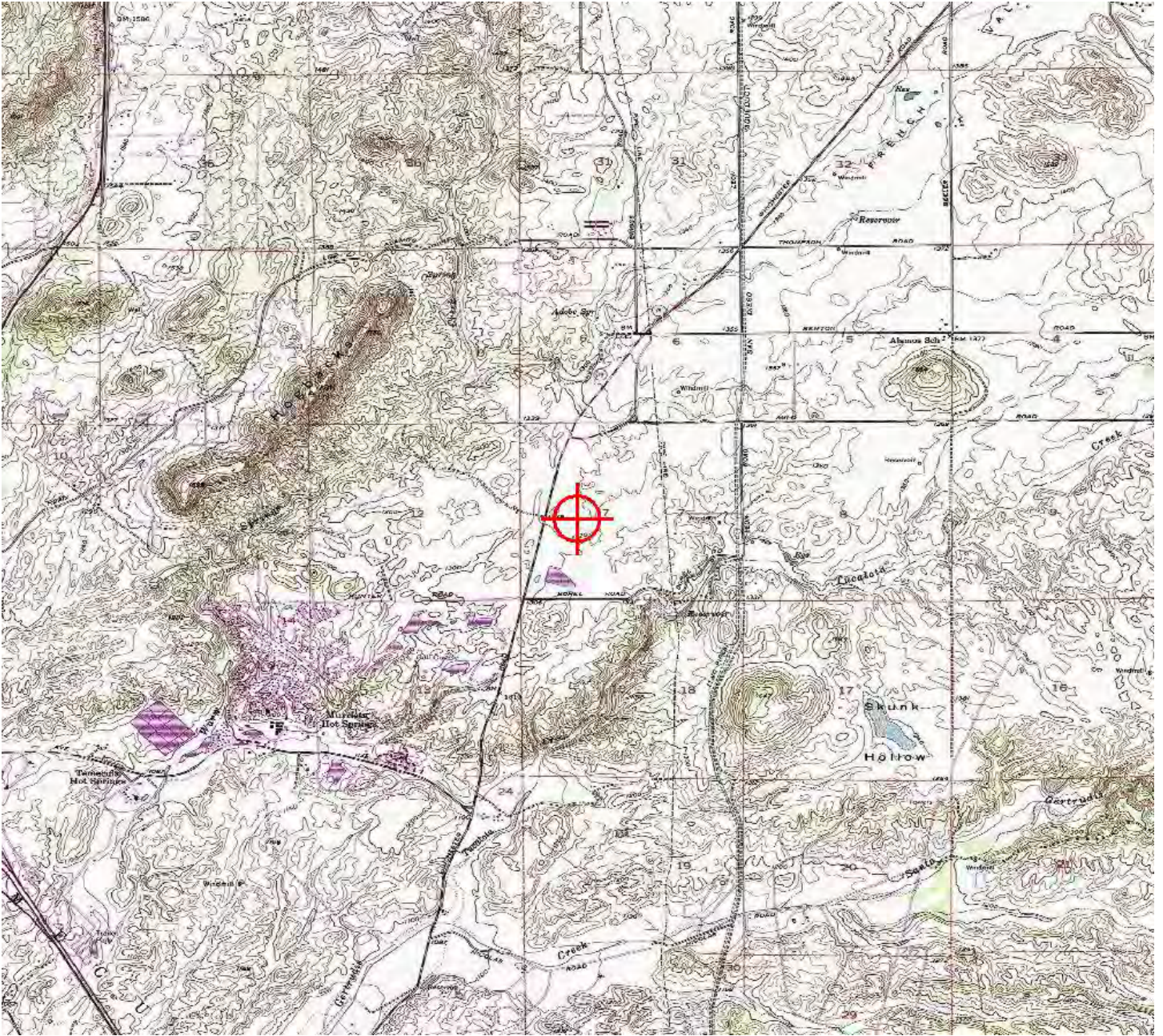
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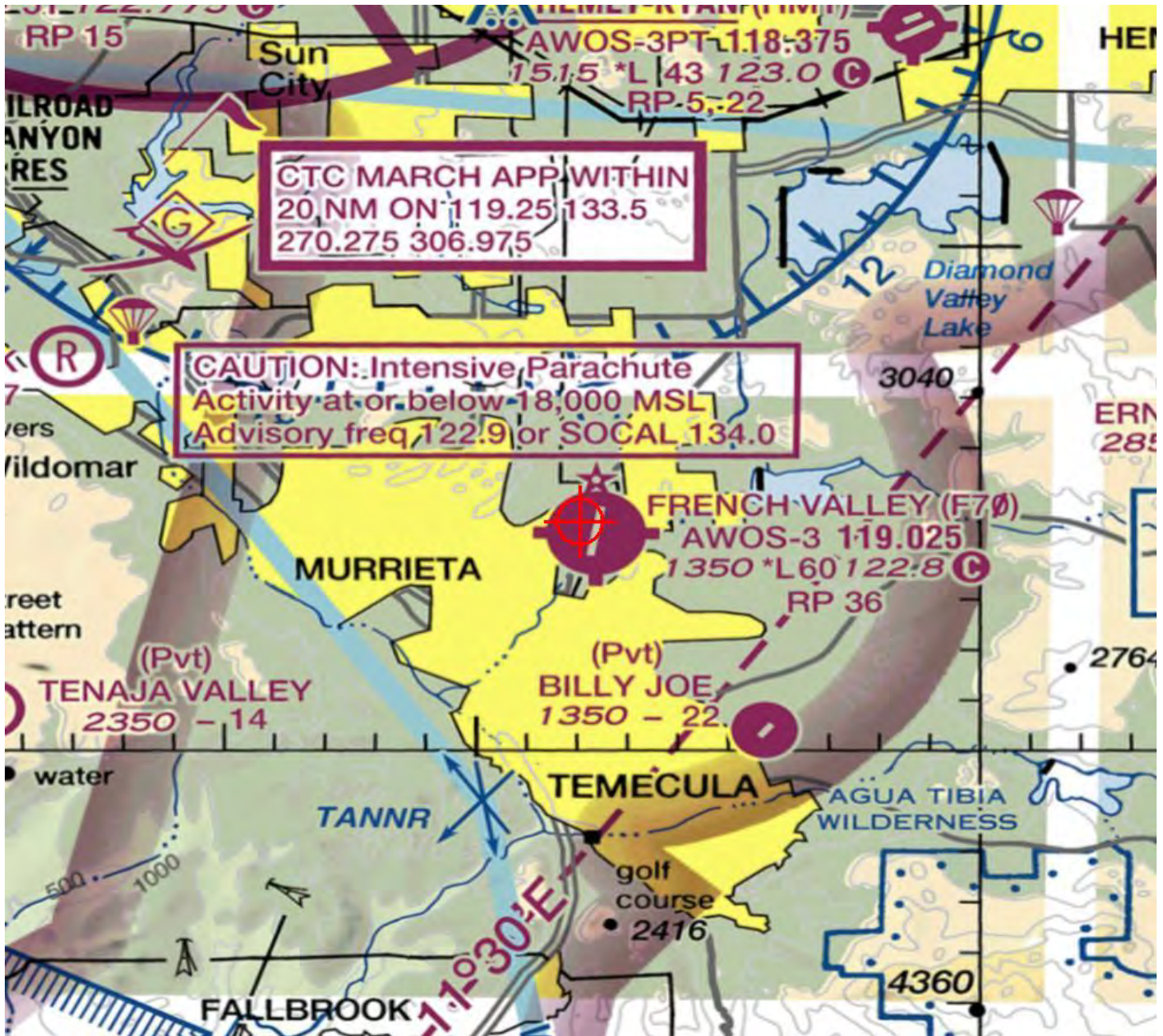
(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-11063-OE







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11064-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 4 - NW
 Location: Murrietta, CA
 Latitude: 33-34-32.32N NAD 83
 Longitude: 117-07-57.52W
 Heights: 1329 feet site elevation (SE)
 50 feet above ground level (AGL)
 1379 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11064-OE.

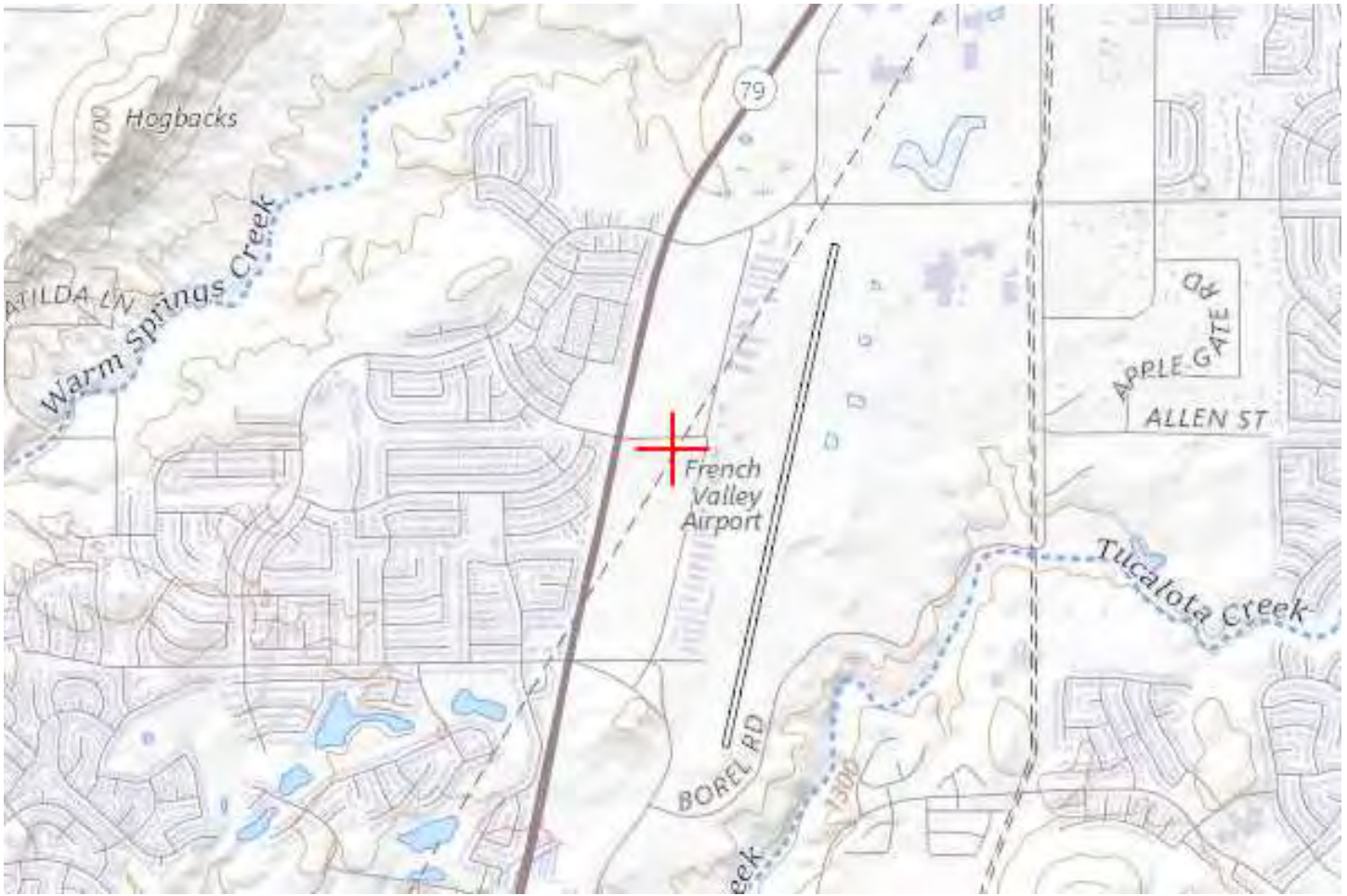
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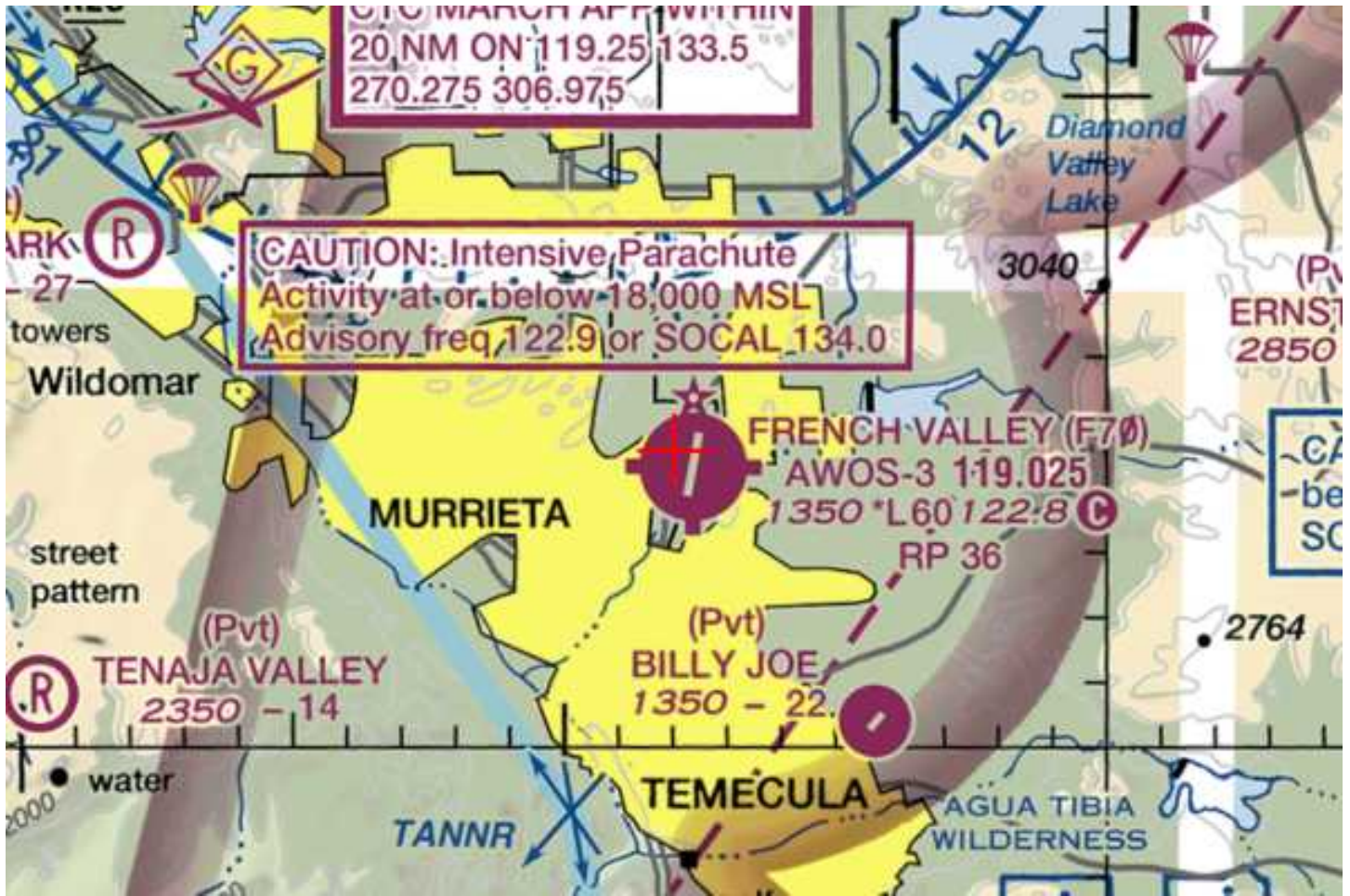
(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-11064-OE







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11065-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 5 - SE
 Location: Murrietta, CA
 Latitude: 33-34-21.39N NAD 83
 Longitude: 117-08-05.89W
 Heights: 1328 feet site elevation (SE)
 52 feet above ground level (AGL)
 1380 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

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This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11065-OE.

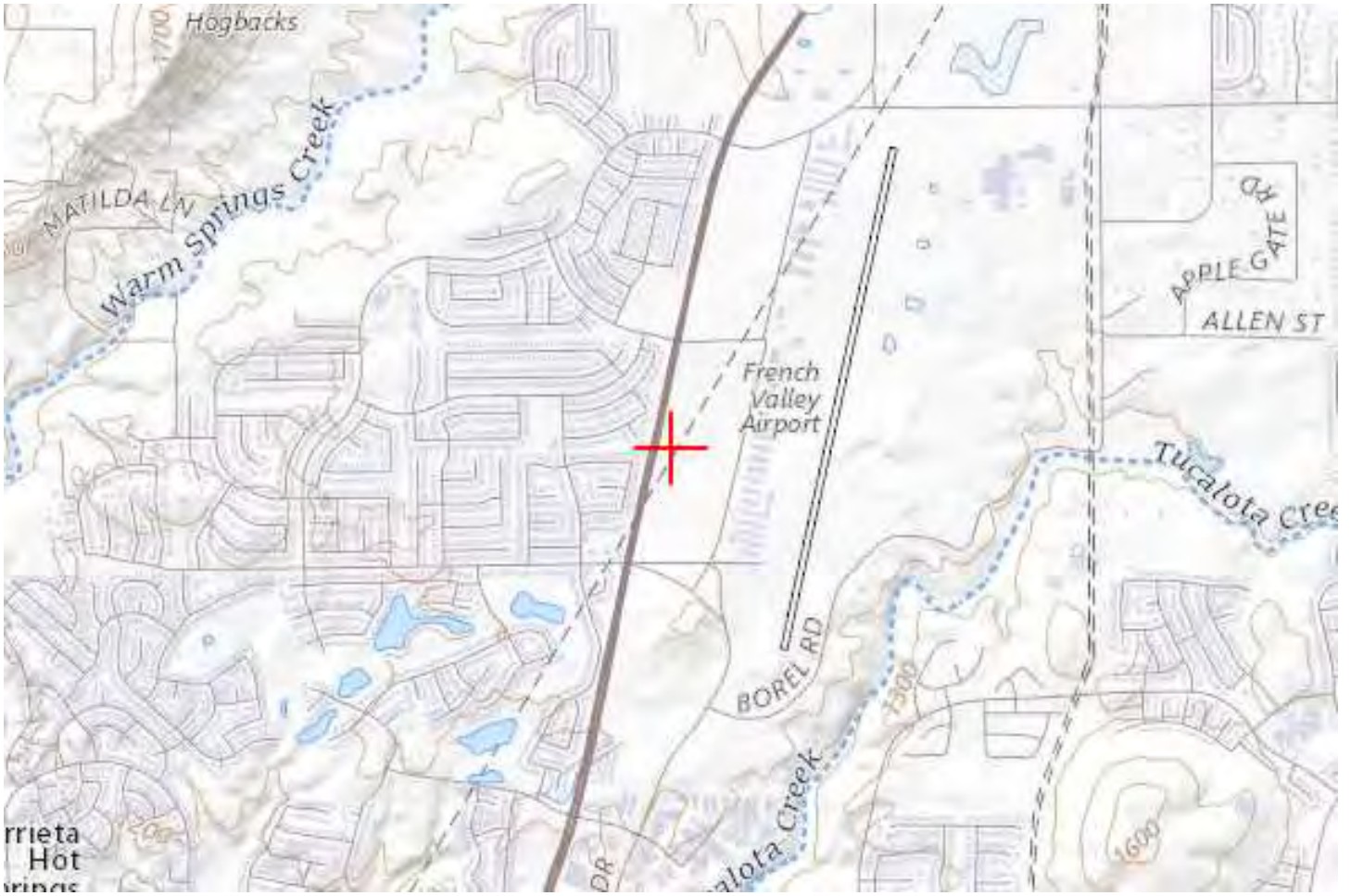
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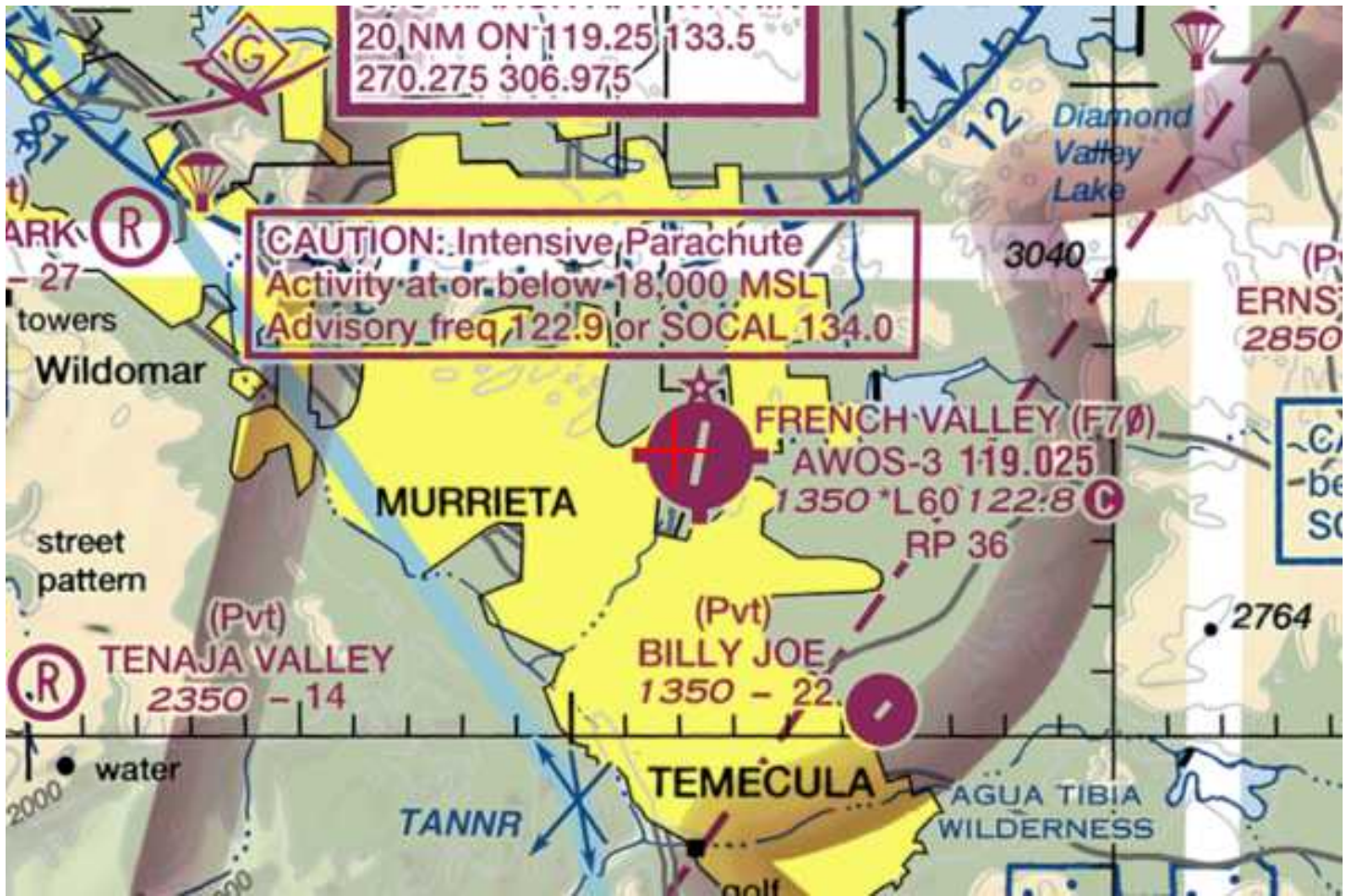
(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-11065-OE







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11066-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 5 - SW
 Location: Murrietta, CA
 Latitude: 33-34-21.48N NAD 83
 Longitude: 117-08-06.48W
 Heights: 1328 feet site elevation (SE)
 52 feet above ground level (AGL)
 1380 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11066-OE.

Signature Control No: 592524683-594010622

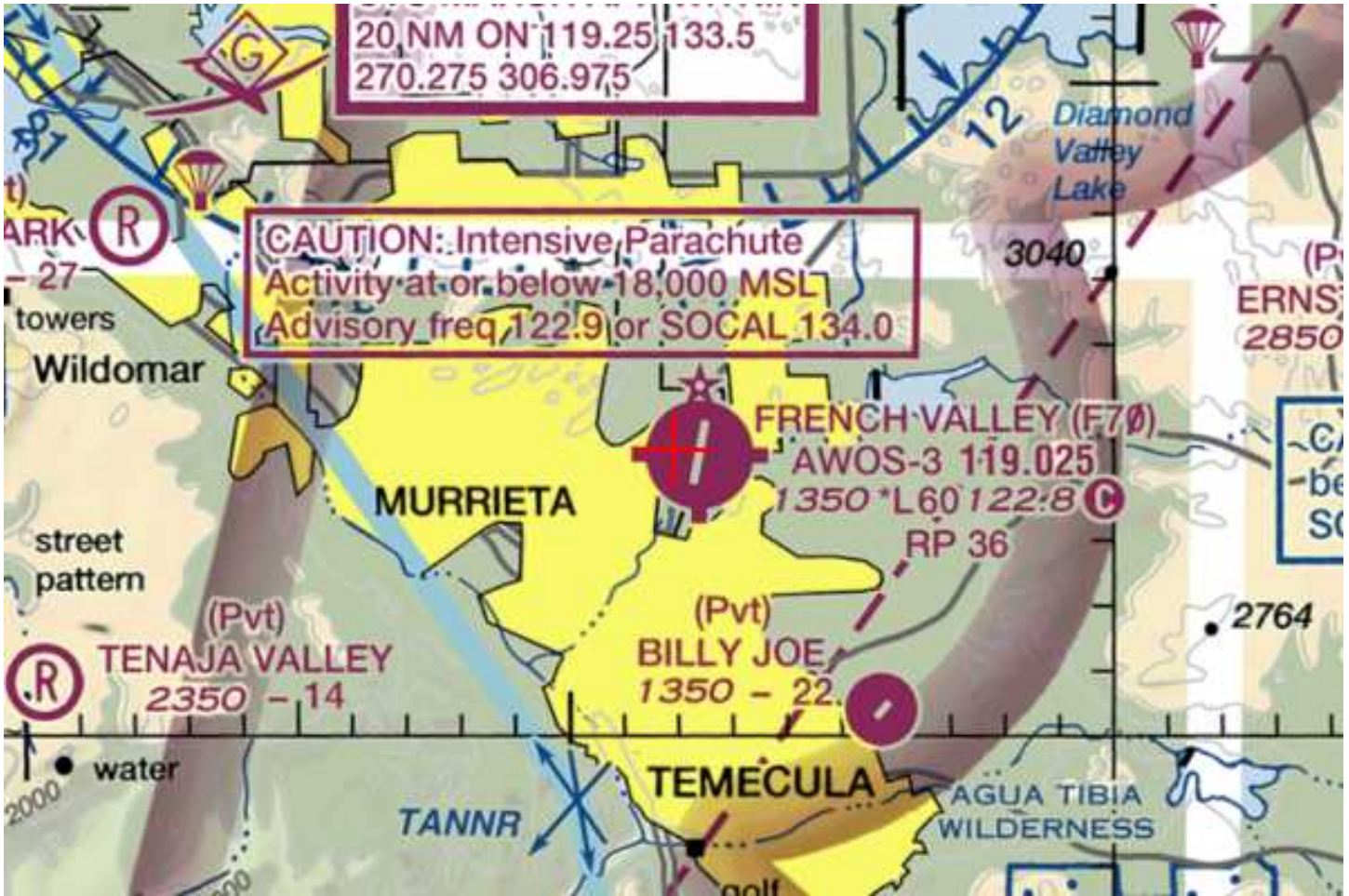
(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-11066-OE







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11067-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 5 - NE
 Location: Murrietta, CA
 Latitude: 33-34-24.15N NAD 83
 Longitude: 117-08-04.20W
 Heights: 1328 feet site elevation (SE)
 52 feet above ground level (AGL)
 1380 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11067-OE.

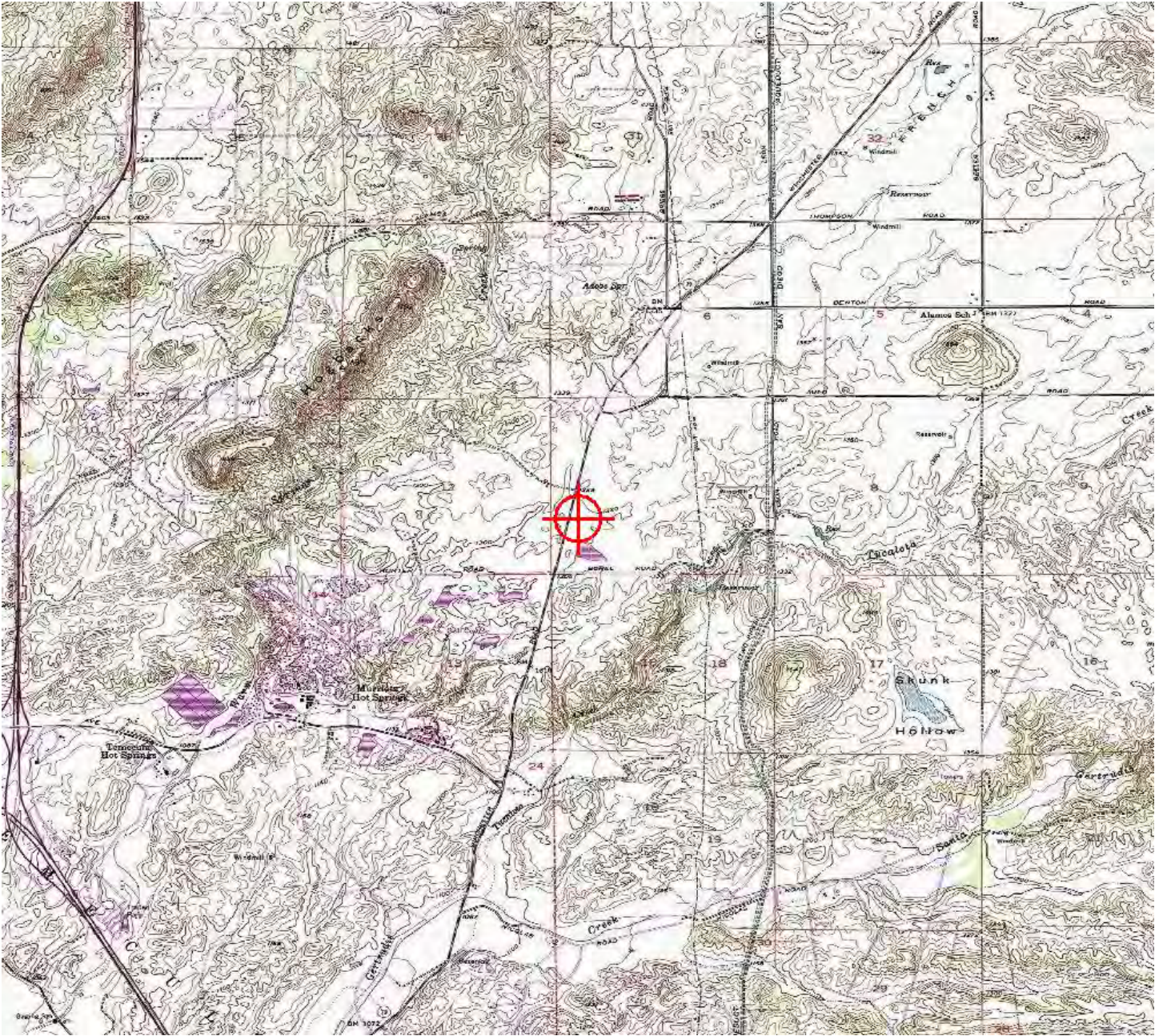
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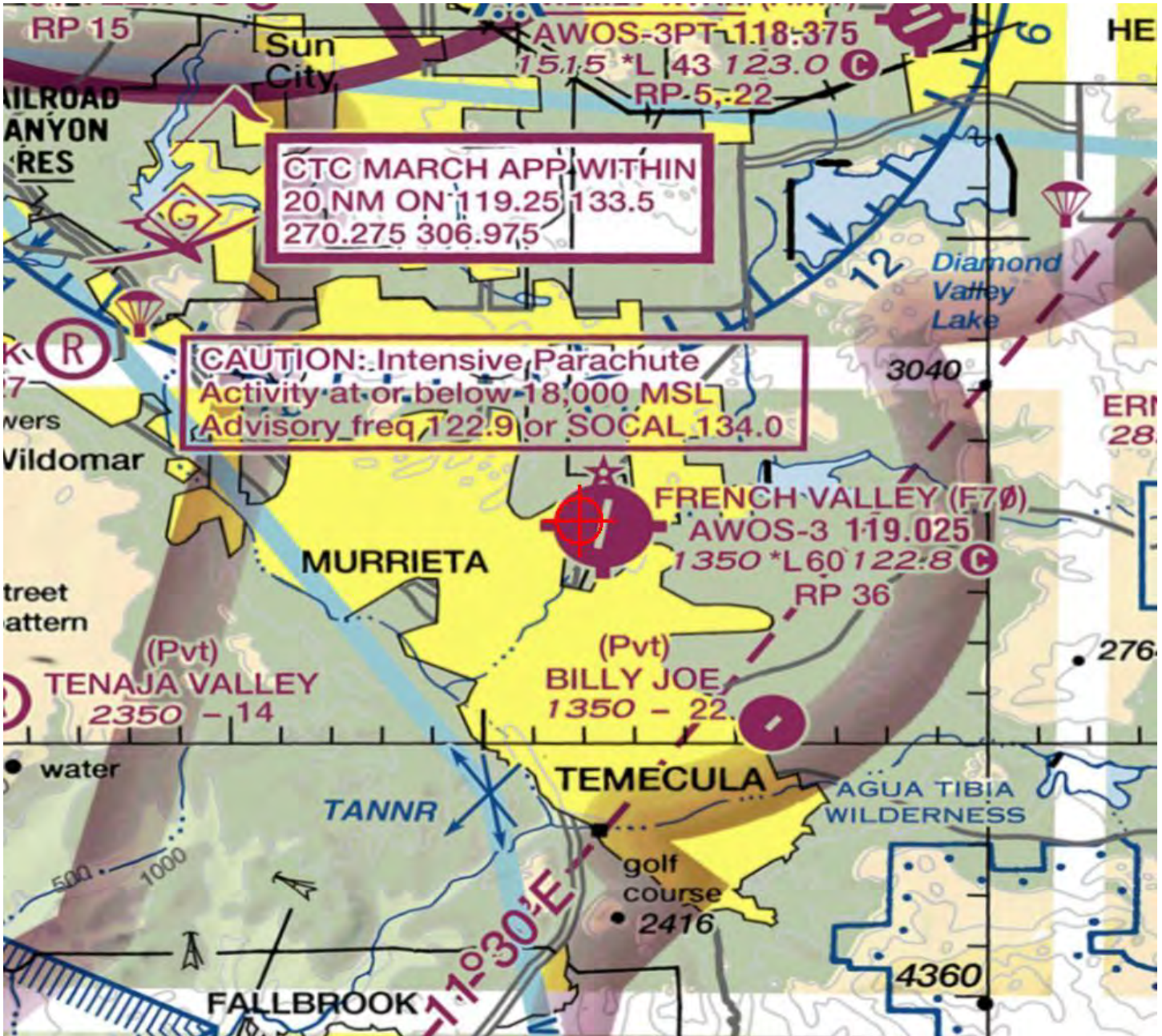
(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-11067-OE







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11068-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 5 - NW
 Location: Murrietta, CA
 Latitude: 33-34-24.40N NAD 83
 Longitude: 117-08-05.73W
 Heights: 1325 feet site elevation (SE)
 52 feet above ground level (AGL)
 1377 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11068-OE.

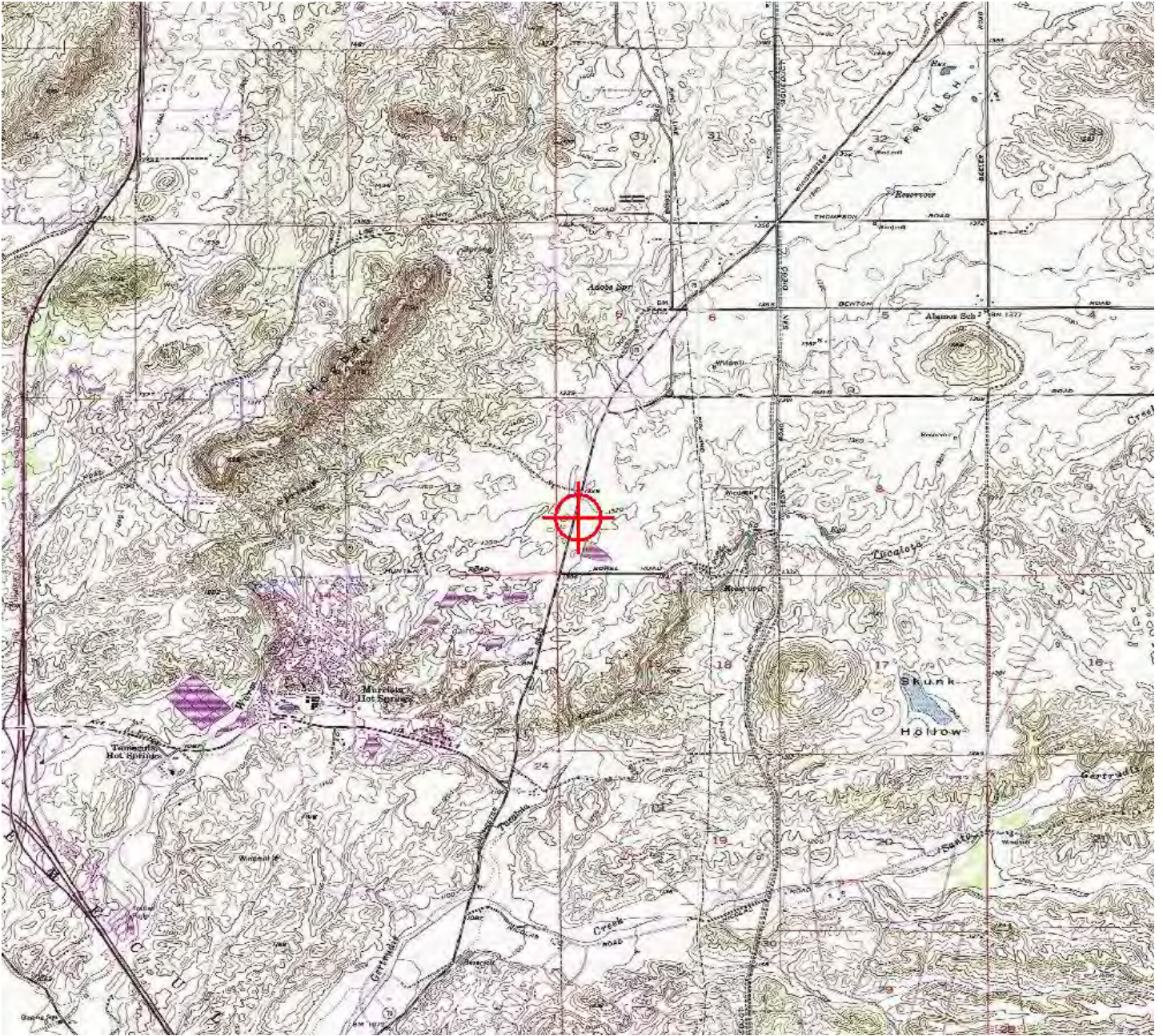
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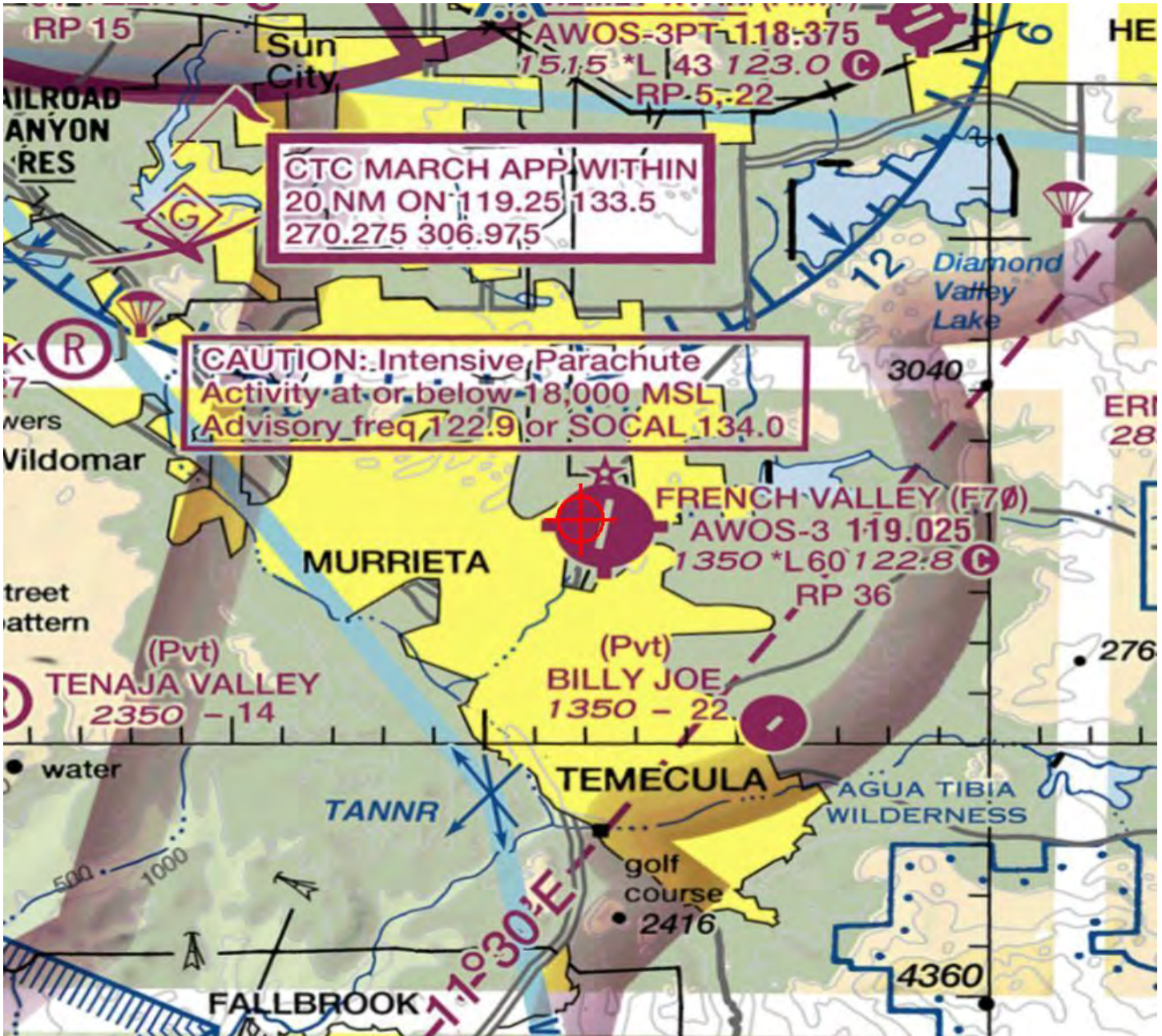
(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-11068-OE







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11069-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 6 - SE
 Location: Murrietta, CA
 Latitude: 33-34-24.97N NAD 83
 Longitude: 117-08-03.81W
 Heights: 1328 feet site elevation (SE)
 41 feet above ground level (AGL)
 1369 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

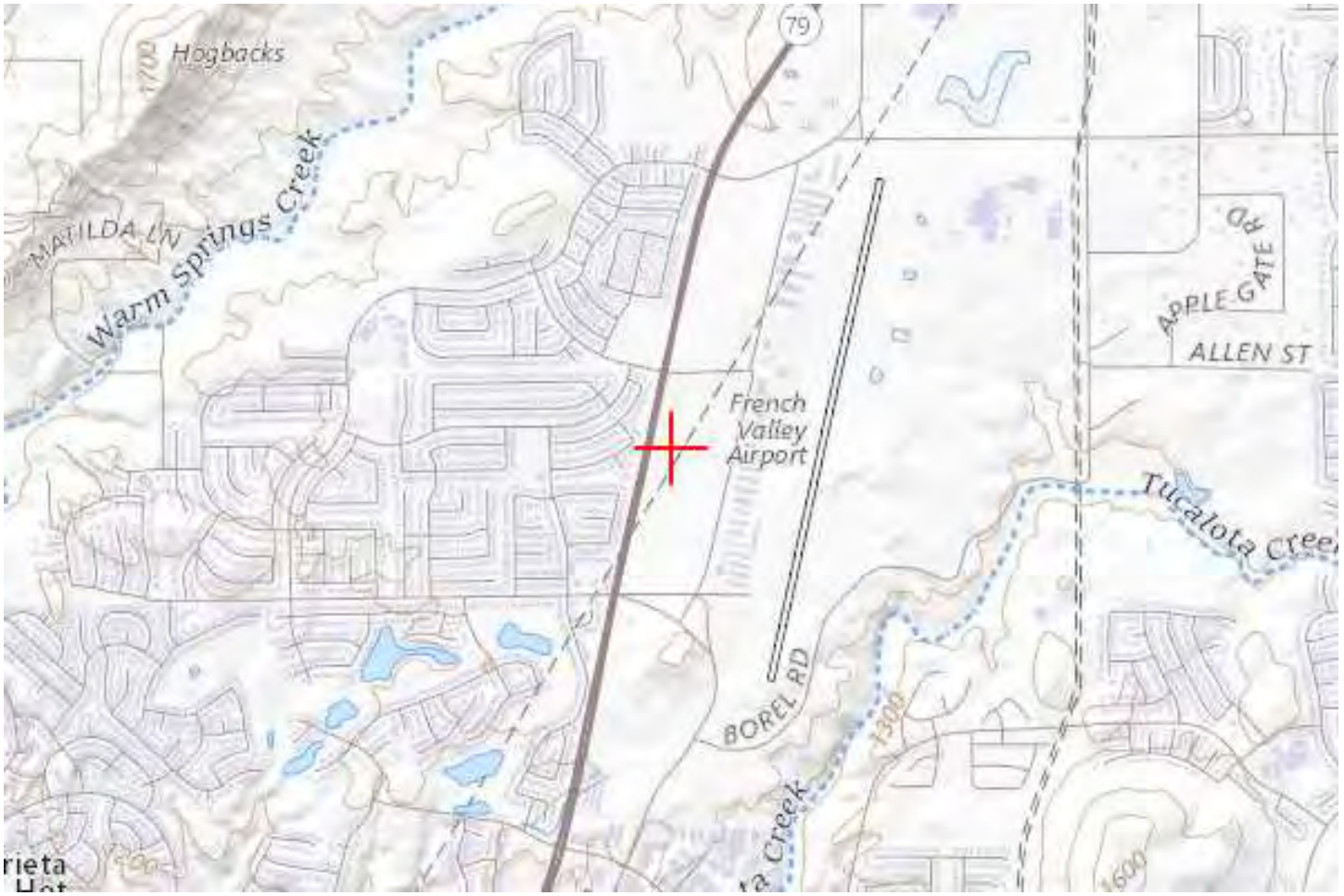
If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11069-OE.

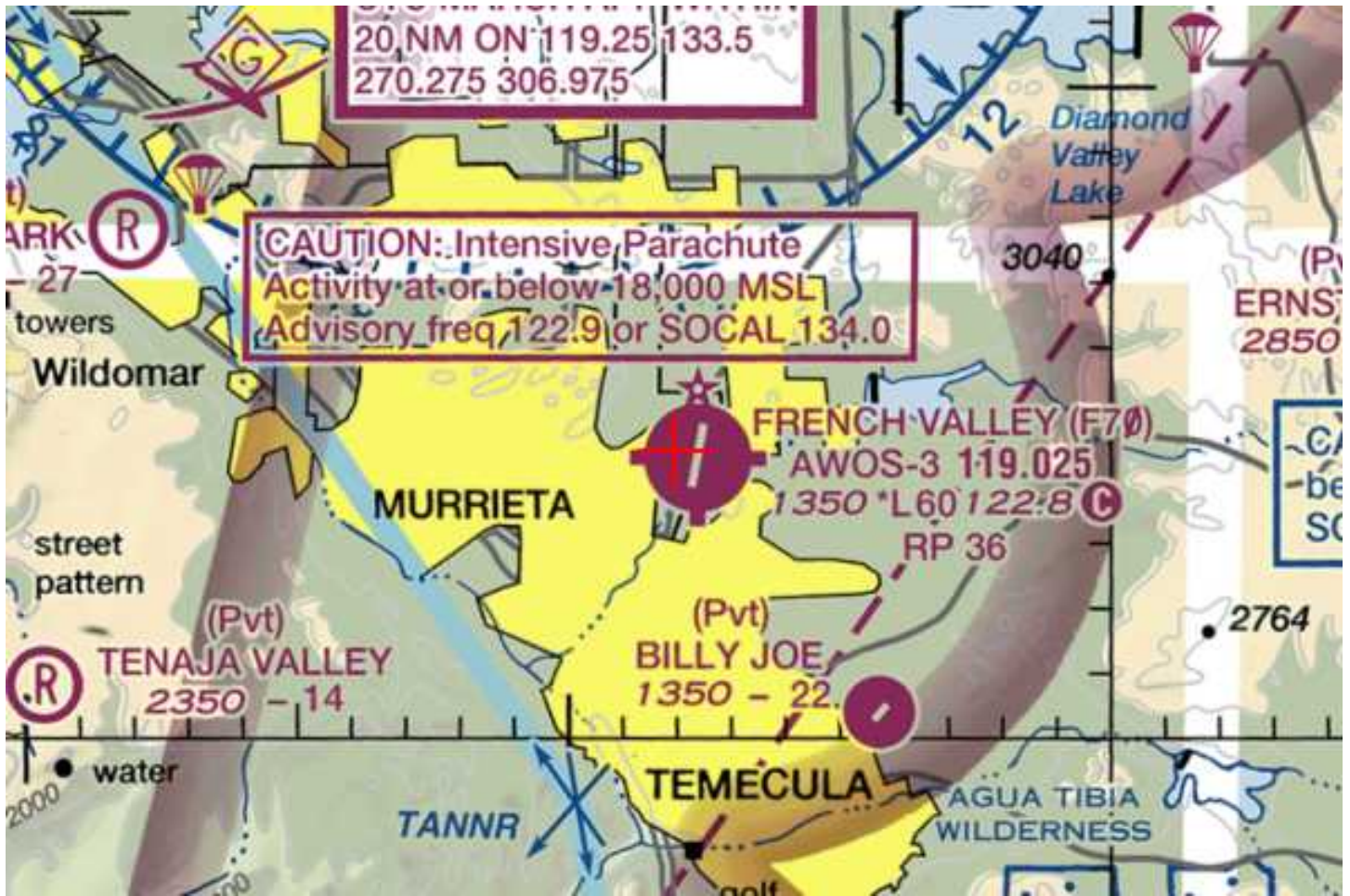
Signature Control No: 592524697-594010659

(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11070-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Commercial Use Building Winchester Building Park - Building 6 - SW
Location:	Murrietta, CA
Latitude:	33-34-25.16N NAD 83
Longitude:	117-08-04.82W
Heights:	1325 feet site elevation (SE)
	41 feet above ground level (AGL)
	1366 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11070-OE.

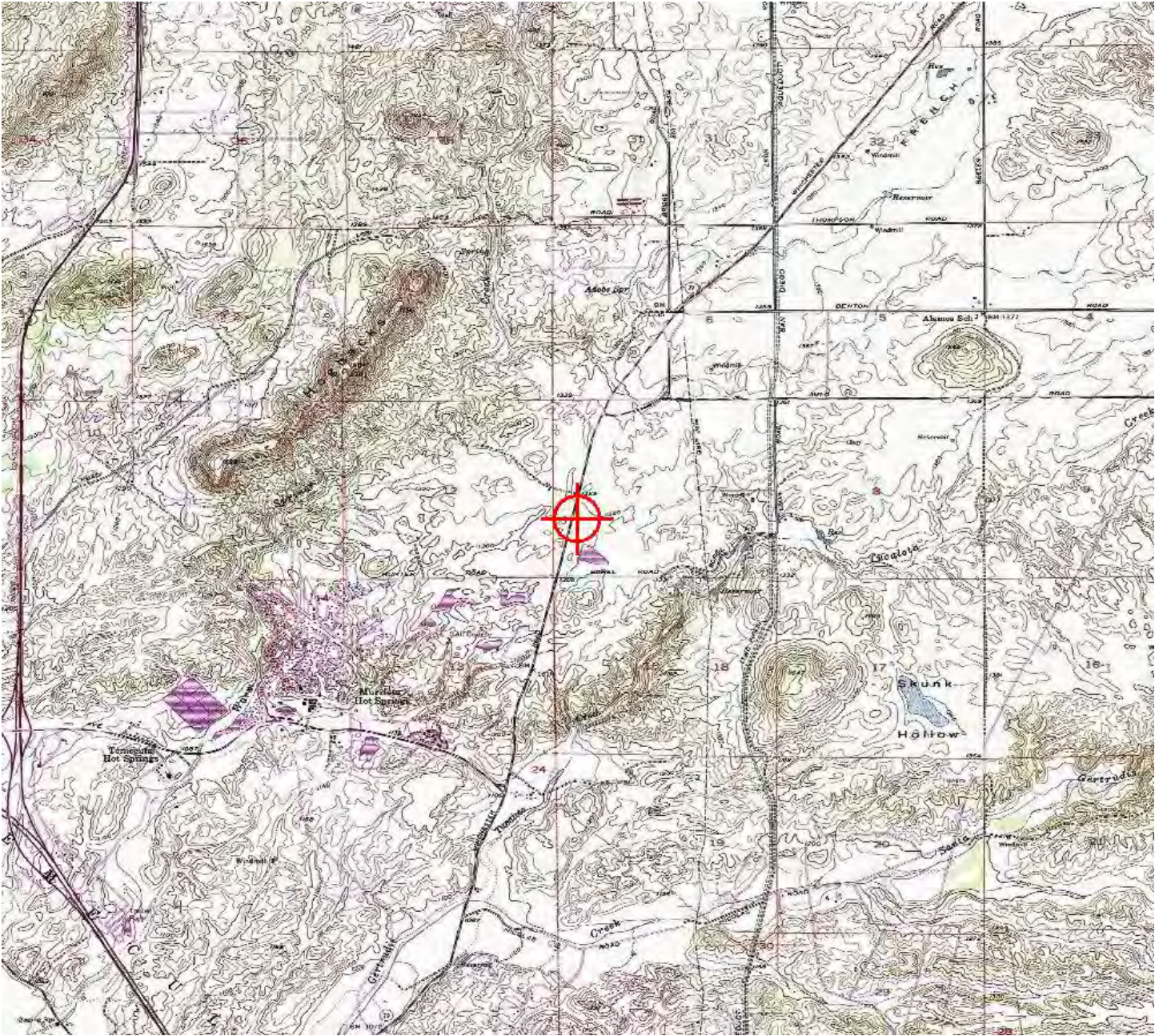
Signature Control No: 592524698-594010664

(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-11070-OE





Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11071-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 6 - NE
 Location: Murrietta, CA
 Latitude: 33-34-27.91N NAD 83
 Longitude: 117-08-03.02W
 Heights: 1330 feet site elevation (SE)
 41 feet above ground level (AGL)
 1371 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11071-OE.

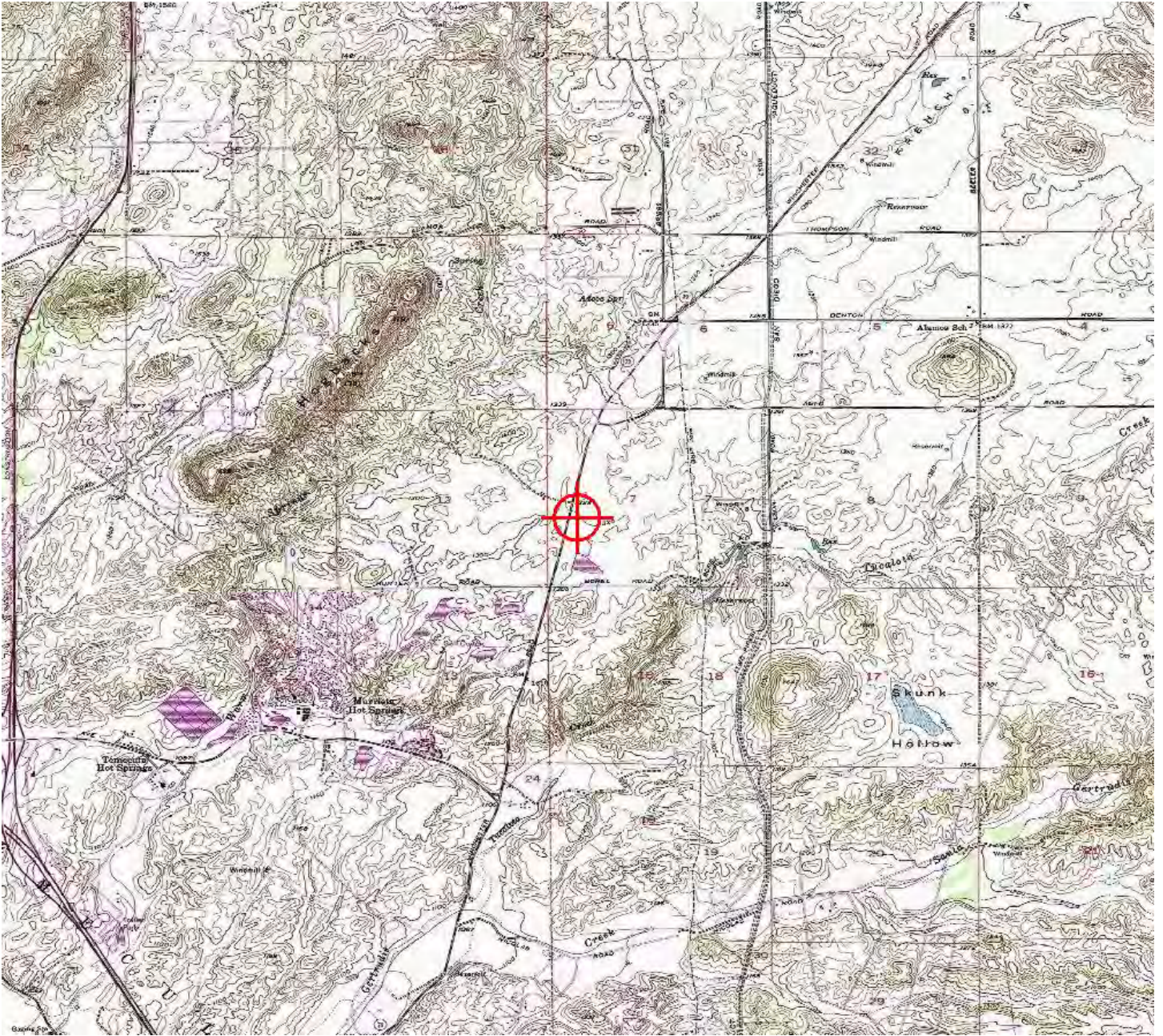
Signature Control No: 592524699-594010666

(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-11071-OE





Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11072-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 6 - NW
 Location: Murrietta, CA
 Latitude: 33-34-28.09N NAD 83
 Longitude: 117-08-04.05W
 Heights: 1333 feet site elevation (SE)
 41 feet above ground level (AGL)
 1374 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

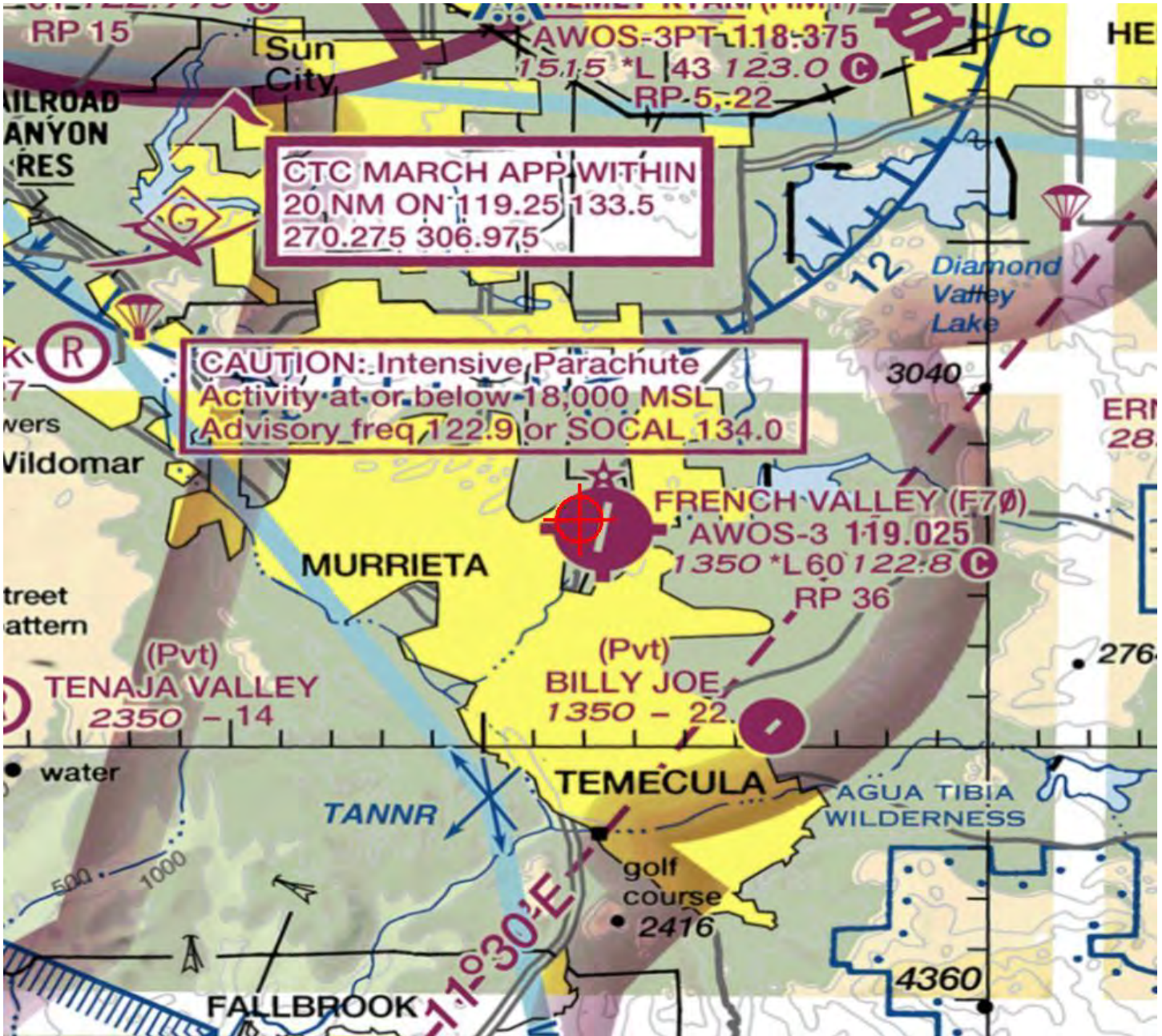
If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11072-OE.

Signature Control No: 592524700-594010669

(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)





Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11073-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 7 - SE
 Location: Murrietta, CA
 Latitude: 33-34-28.82N NAD 83
 Longitude: 117-08-02.78W
 Heights: 1328 feet site elevation (SE)
 41 feet above ground level (AGL)
 1369 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11073-OE.

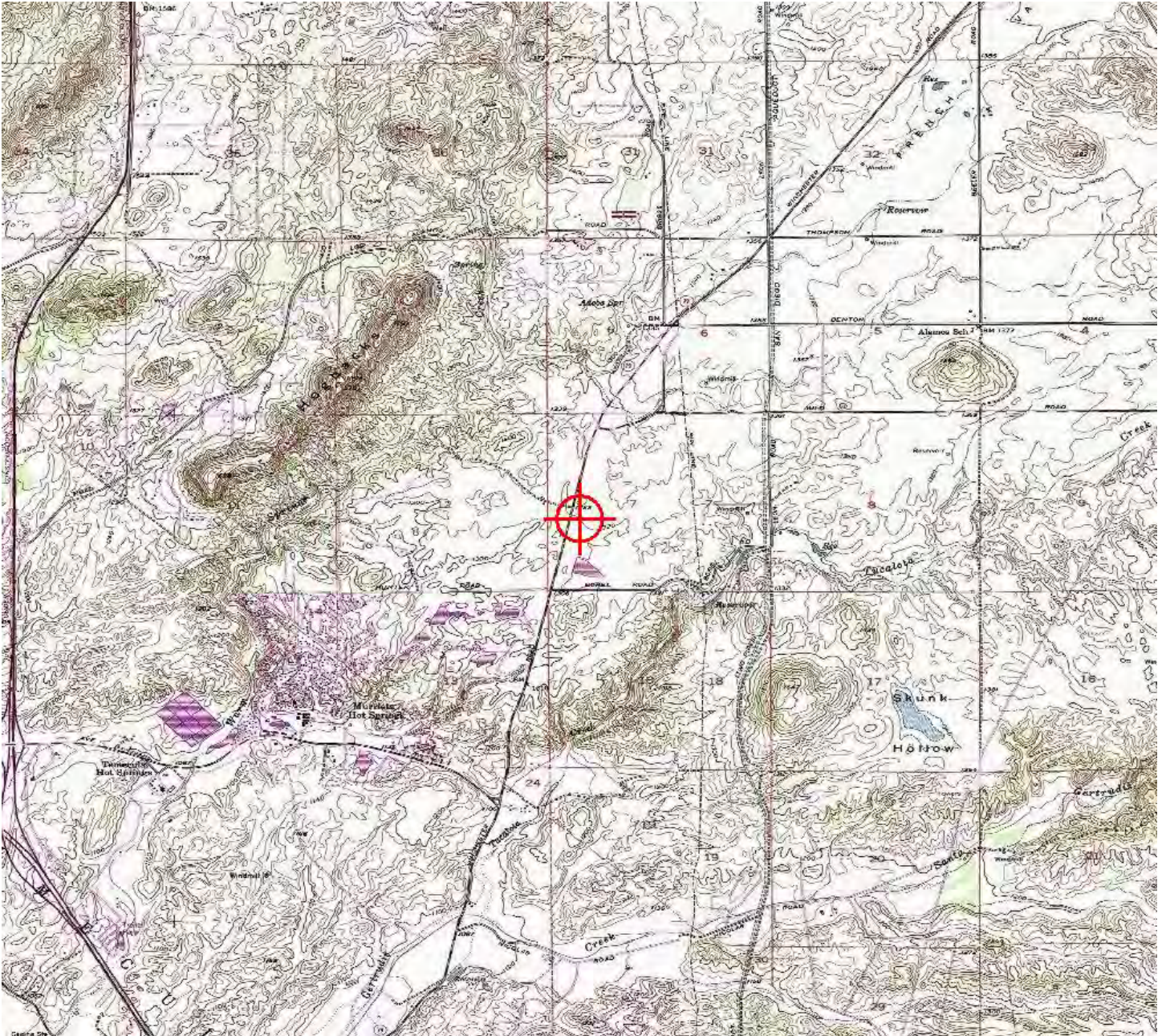
Signature Control No: 592524707-594010676

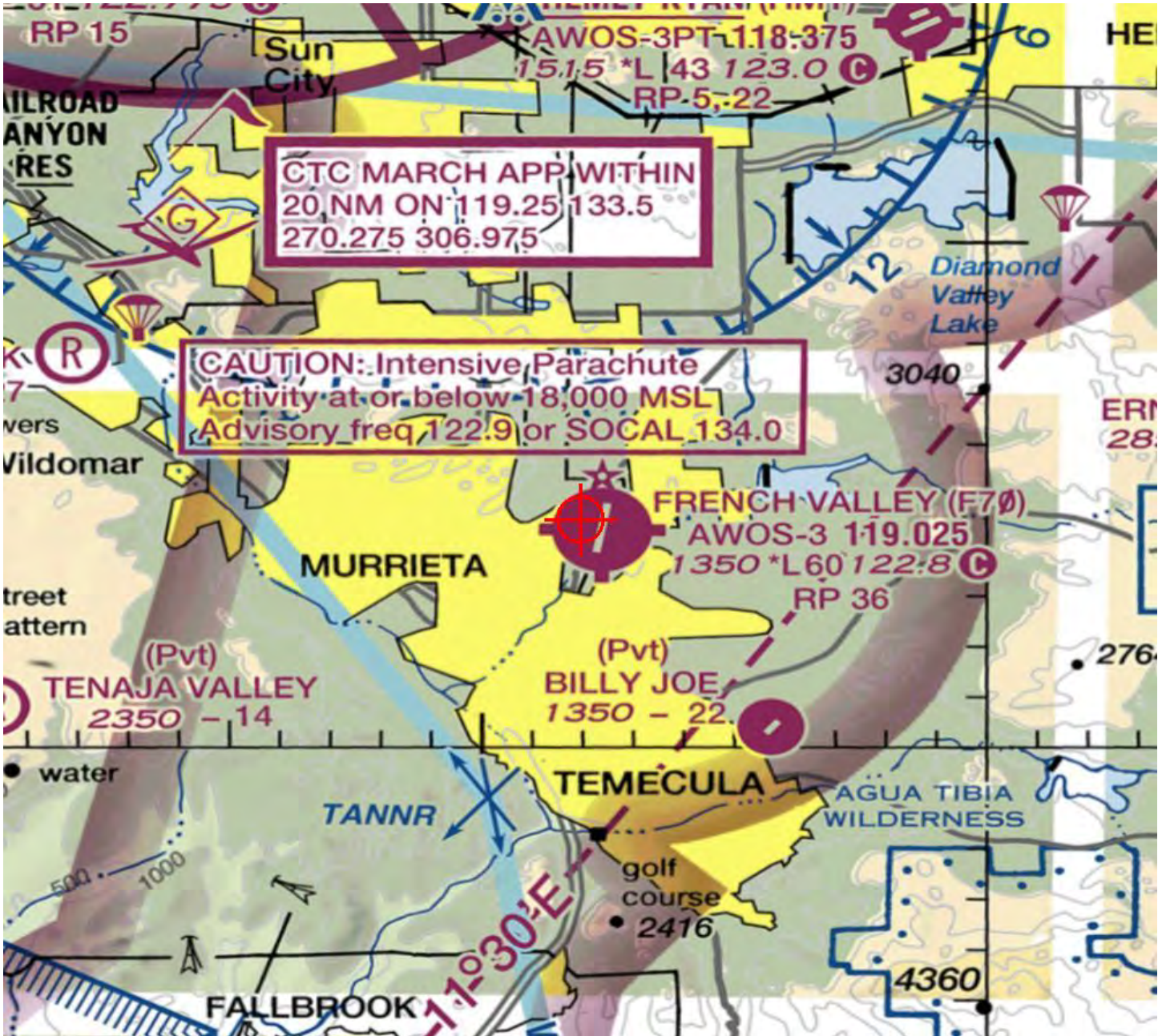
(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-11073-OE







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11074-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 7 - SW
 Location: Murrietta, CA
 Latitude: 33-34-29.00N NAD 83
 Longitude: 117-08-03.76W
 Heights: 1330 feet site elevation (SE)
 41 feet above ground level (AGL)
 1371 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11074-OE.

Signature Control No: 592524708-594010681

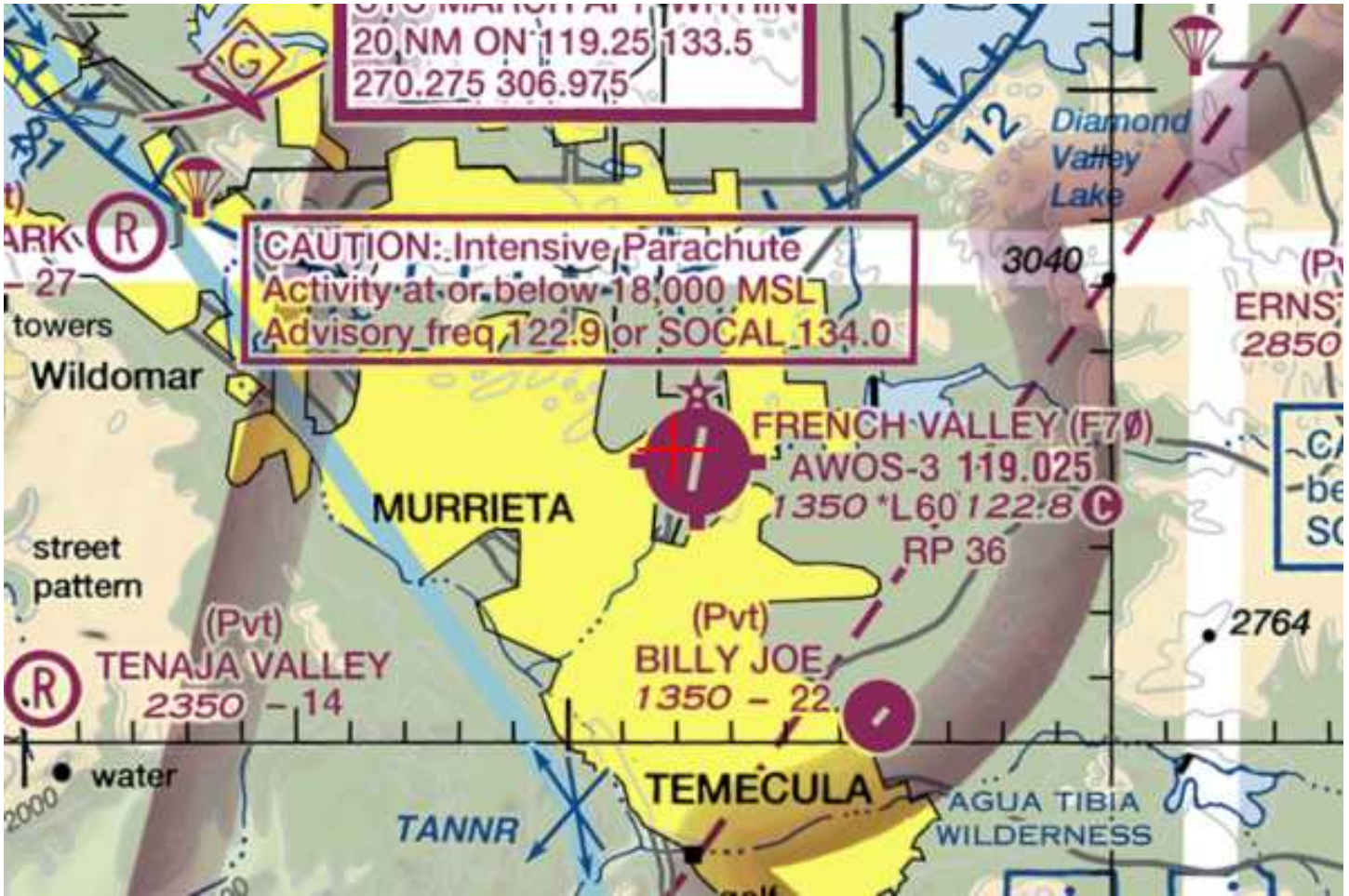
(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2023-AWP-11074-OE







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11075-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Commercial Use Building Winchester Building Park - Building 7 - NE
Location:	Murrietta, CA
Latitude:	33-34-31.78N NAD 83
Longitude:	117-08-02.02W
Heights:	1326 feet site elevation (SE) 41 feet above ground level (AGL) 1367 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

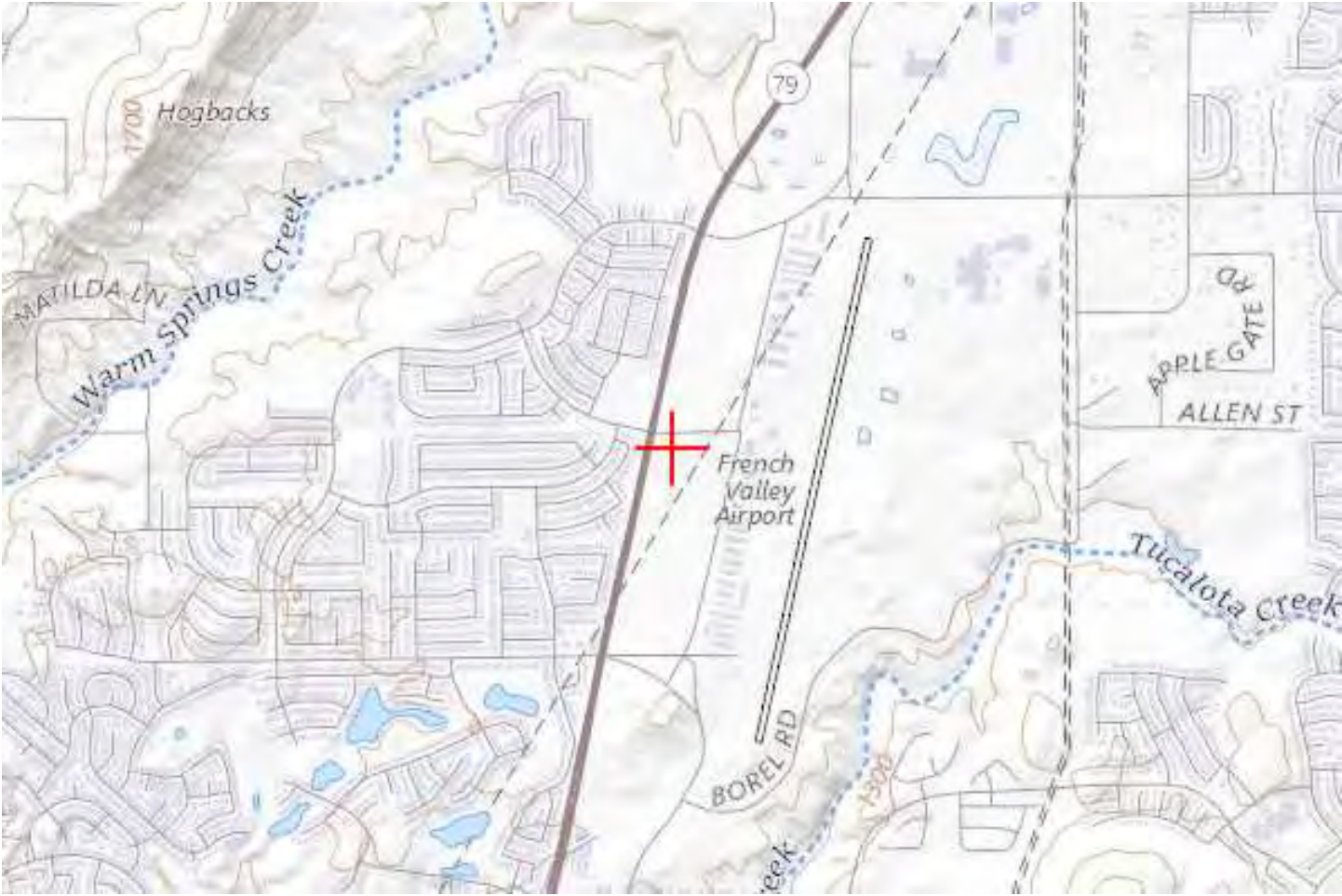
If we can be of further assistance, please contact our office at (847) 294-7575, or vivian.vilaro@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2023-AWP-11075-OE.

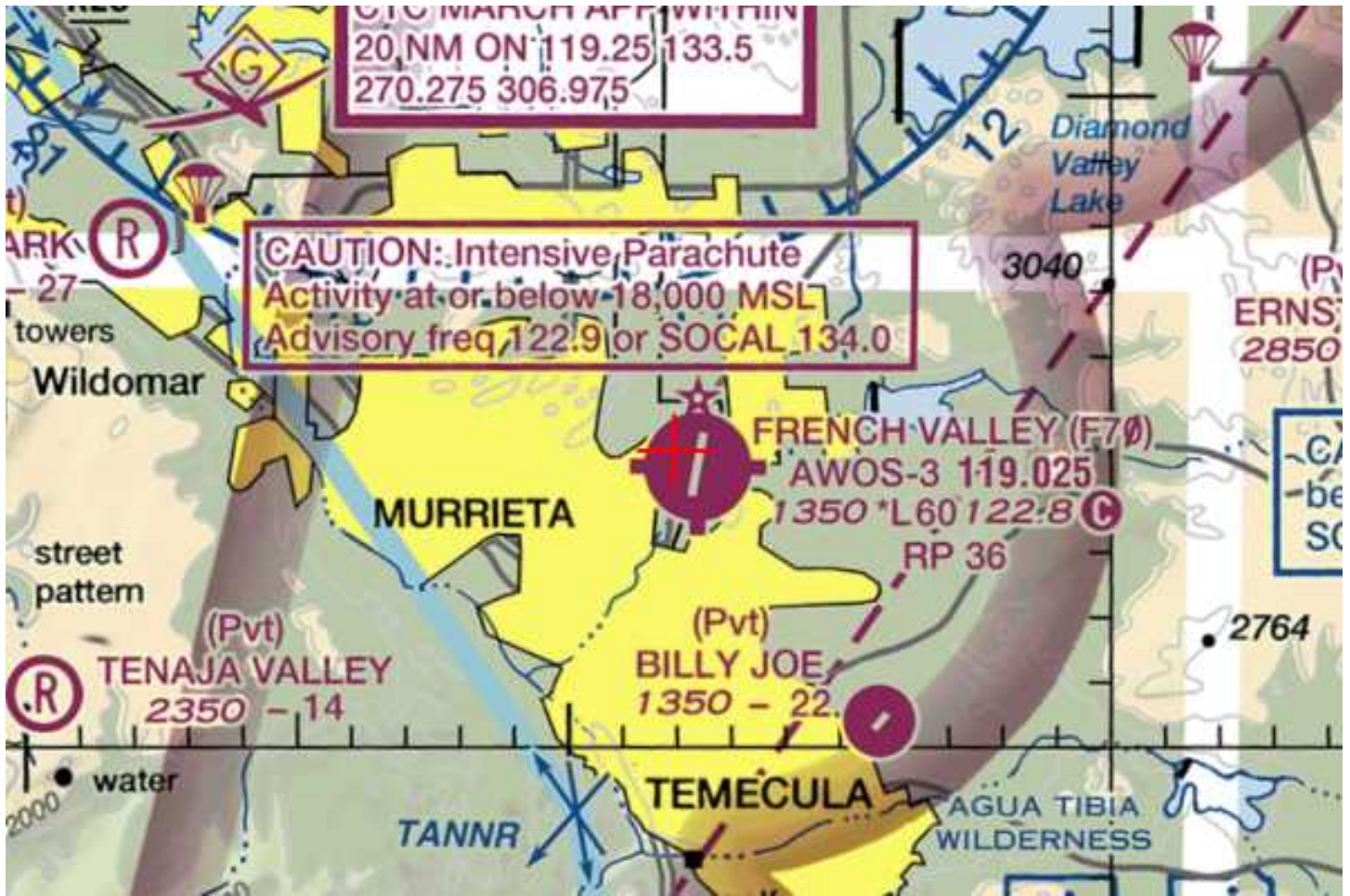
Signature Control No: 592524709-594010682

(DNE)

Vivian Vilaro
Specialist

Attachment(s)
Map(s)







Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2023-AWP-11076-OE

Issued Date: 07/20/2023

Ms. Brandi Self
 Xebec - Winchester Business Park 1-7
 3020 Old Ranch Parkway - Suite 200
 Sponsor Representative - Mrs. Paula Kelly
 Seal Beach, CA 90740

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Winchester Building Park - Building 7 - NW
 Location: Murrietta, CA
 Latitude: 33-34-31.97N NAD 83
 Longitude: 117-08-03.04W
 Heights: 1325 feet site elevation (SE)
 41 feet above ground level (AGL)
 1366 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/20/2025 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

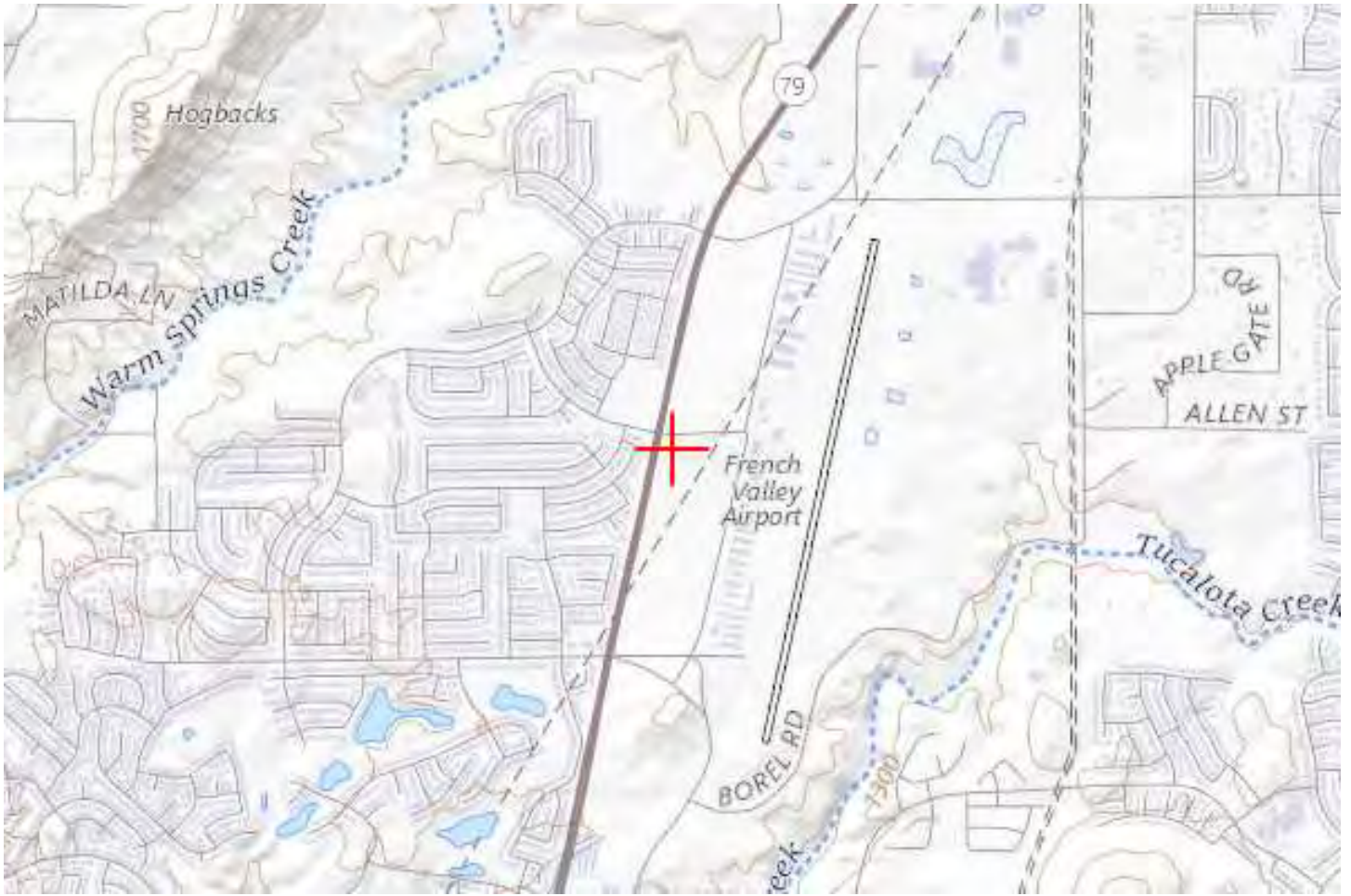
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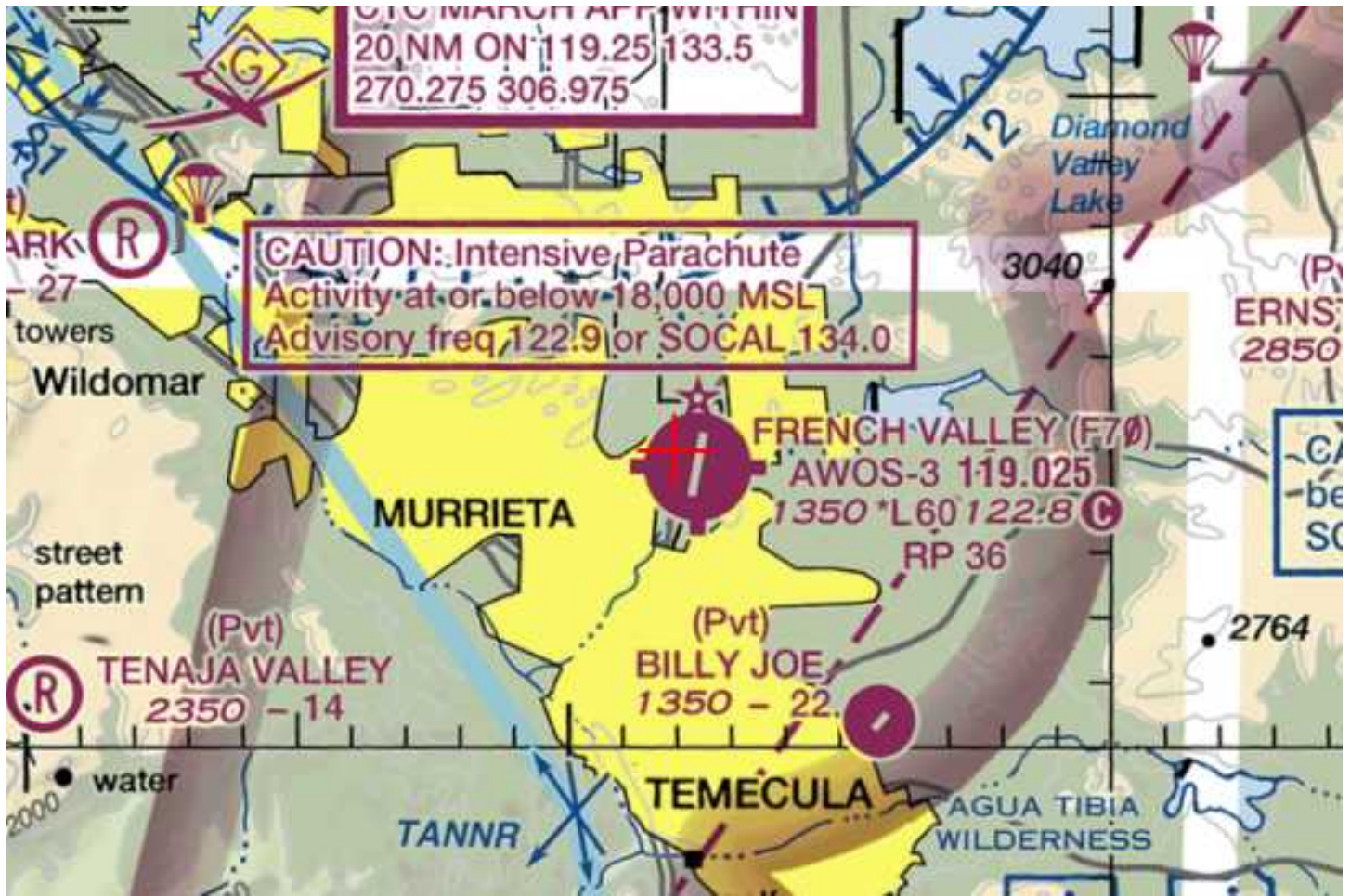
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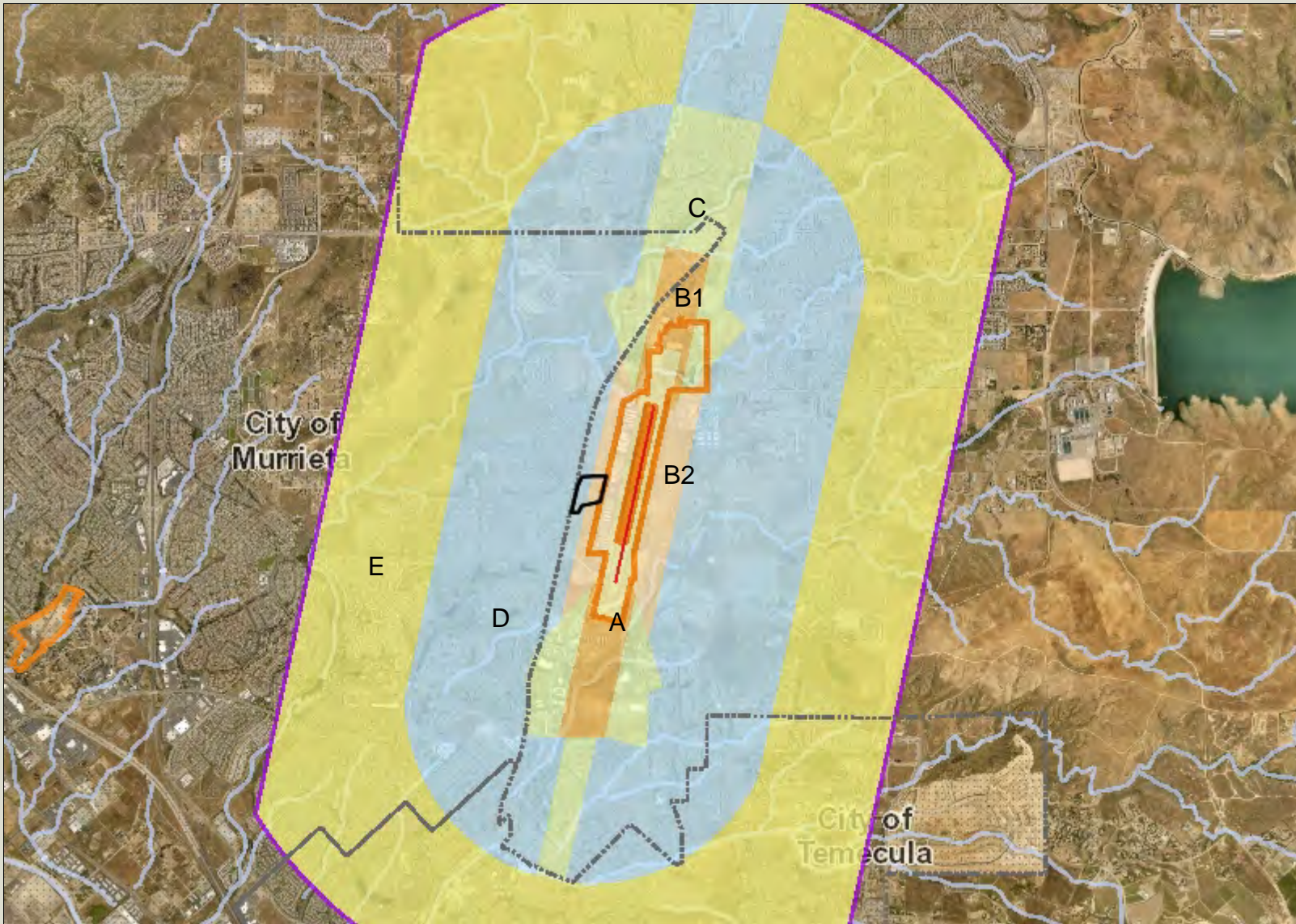
Vivian Vilaro
Specialist

Attachment(s)
Map(s)





Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas

Airport Compatibility Zones

- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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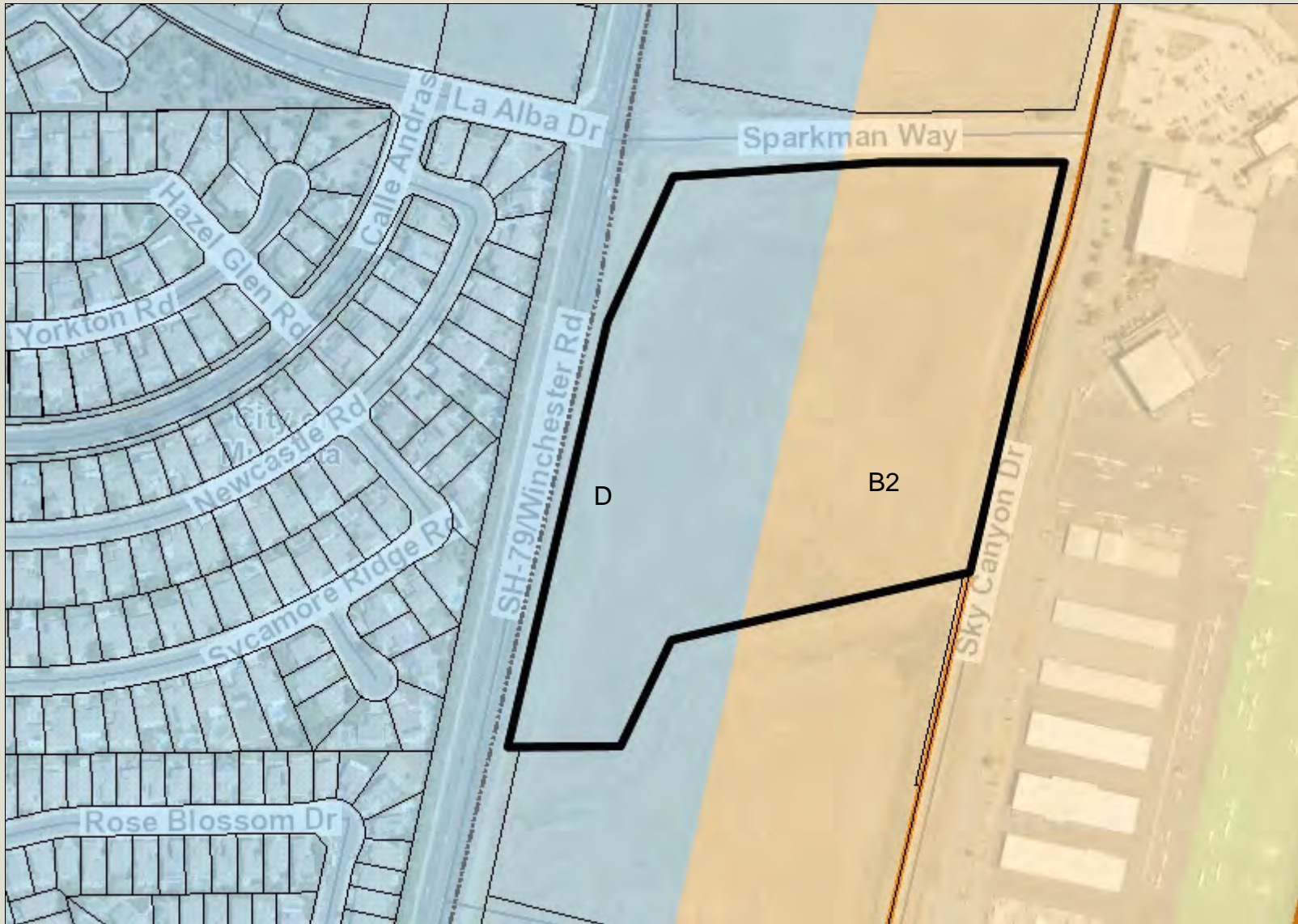


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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas

Airport Compatibility Zones

- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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Notes

Map My County Map



- Legend**
- County Boundary
 - City Boundaries
 - Parcel APNs
 - Parcels
 - Street Names
 - Street Centerlines
 - Blueline Streams
 - City Areas
 - World Street Map



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0 770 1,539 Feet

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Notes

Map My County Map



Legend

-  County Boundary
-  City Boundaries
- Street Names
-  Street Centerlines
-  Blueline Streams
-  City Areas
- World Street Map



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- City Boundaries
- Parcel APNs
- Parcels
- Street Names
- Street Centerlines
- Blueline Streams
- City Areas
- World Street Map



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- City Boundaries
- Blueline Streams
- City Areas
- World Street Map



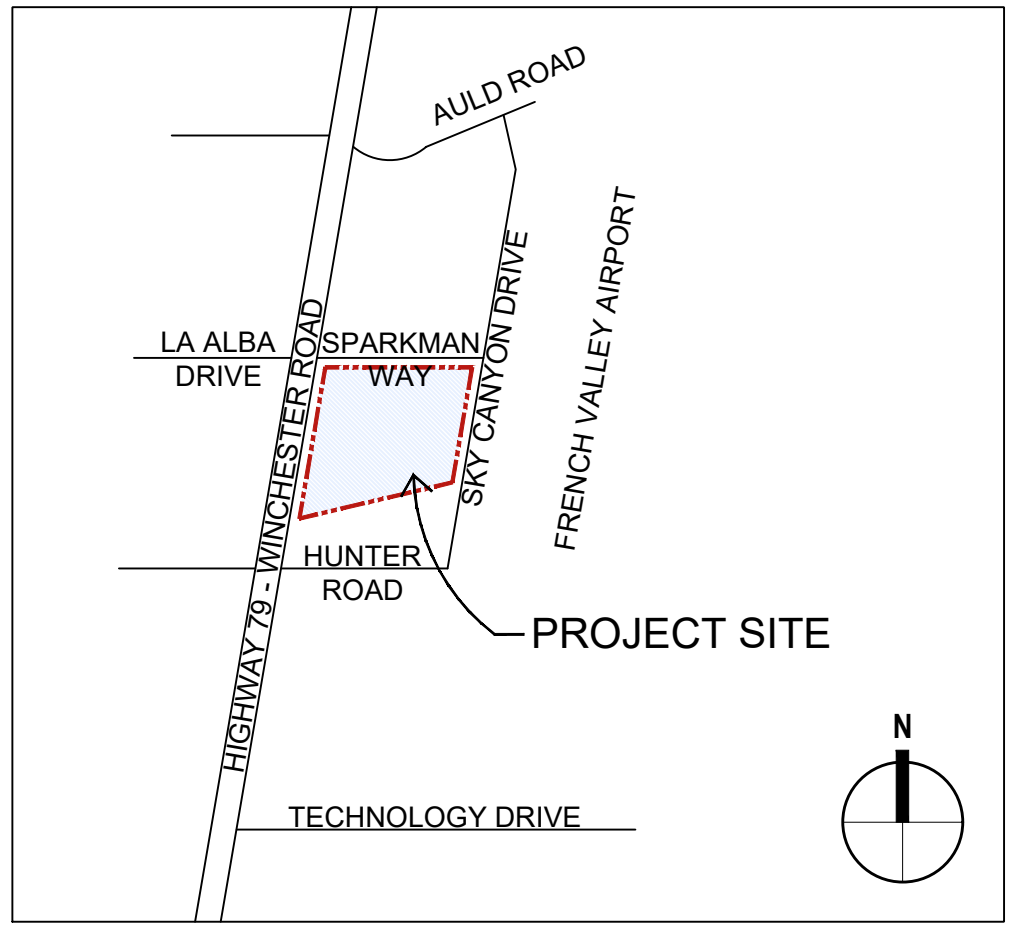
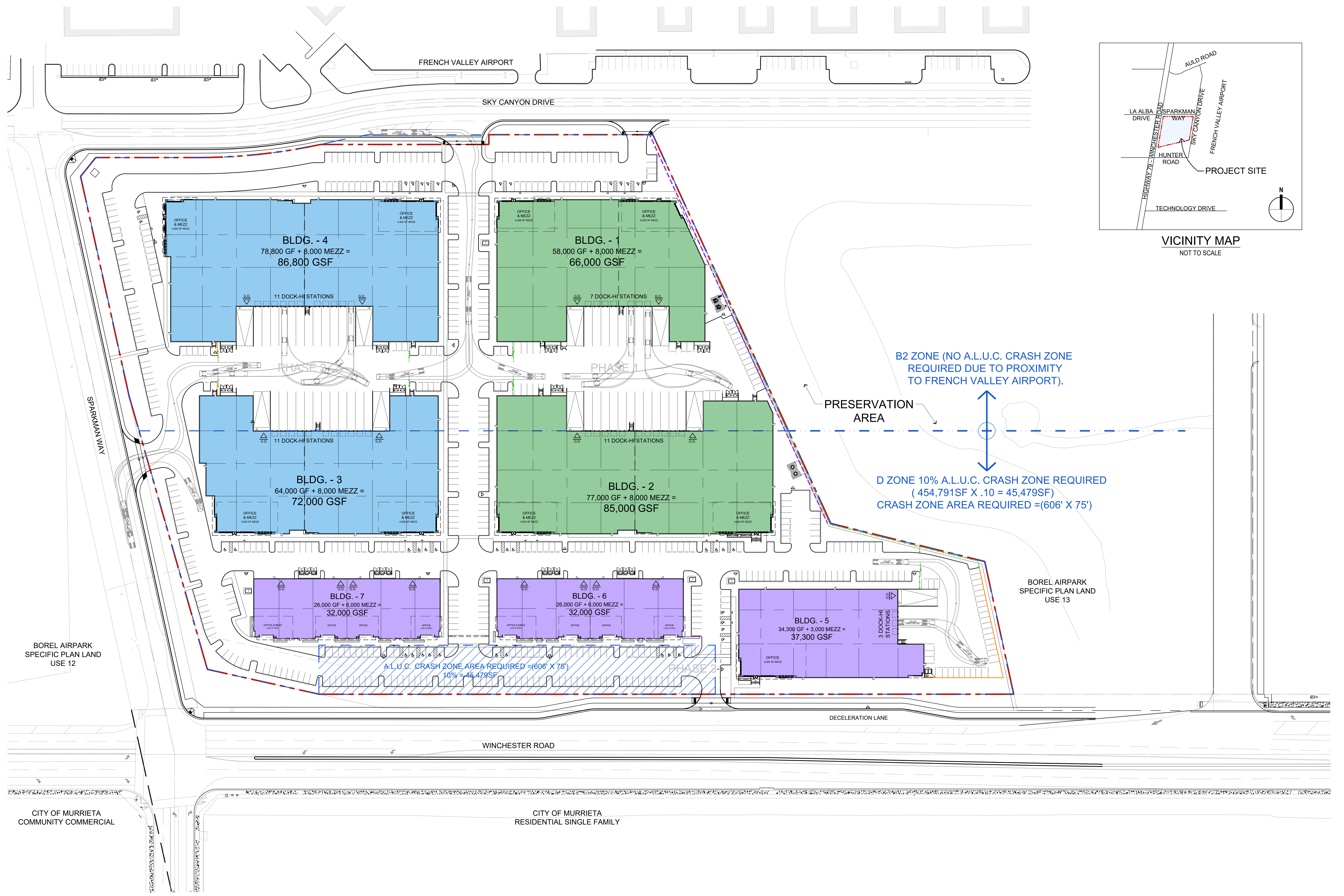
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.



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Notes



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 drawings filename: p:2022-1108-winc-architect-murrieta-03-design-2022-1108-winc-architect-murrieta-master-plan.dwg
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Winchester Road Business Park

Project Description

June 9, 2023

Xebec Building Company (“Applicant”) proposes the development of the Winchester Road Business Park Project on approximately 23.1 acres of vacant land located at the southeast corner of the Winchester Road and Sparkman Way intersection (“Project”) in the County of Riverside, *see Figure 1: Regional Vicinity*. The Project area consists of one (1) irregular-shaped parcel: Assessor’s Parcel Number (APN) 963-030-012, *see Figure 2: Assessor’s Parcel Number*. The Project includes the submittal of a General Plan Amendment (GPA), Specific Plan Amendment (SPA), Change of Zone (CZ), and Plot Plan (PP) applications.

The Project site is located within the French Valley community in unincorporated Riverside County, adjacent to Highway 79 (SR-79), otherwise known as Winchester Road, south of Sparkman Way, north of Borel Road, and west of Sky Canyon Drive, *see Figure 3: Project Site Aerial Imagery*. The Project is adjacent to the French Valley Airport and is located within the French Valley Airport Master Plan, Zones B2 and D. Surrounding uses include vacant property to the north, French Valley Airport to the east, vacant property and KTM North America Inc. to the south, and single-family residential development to the west in the City of Murrieta.

The current General Plan Land Use designation for the Project site is Commercial Retail (CR) and Commercial Office (CO), *see Figure 4: Existing General Plan Land Use Designation*. The Project site is located within the Specific Plan zoning classification, *see Figure 5: Zoning Designation*. The northern portion of the Project site is within Planning Area 12 and the southern portion of the Project site is within Planning area 8 of Specific Plan No. 265.

The following applications for the Project are proposed:

- A General Plan Amendment (GPA) to change the Specific Plan Land Use designation from Commercial Retail (CR) and Commercial Office (CO) to Light Industrial (LI) for APN 963-030-012, *see Figure 6: Proposed General GPA (with a Specific Plan)*.
- A Specific Plan Amendment (SPA) to the Borel Airpark Specific Plan (Specific Plan 265A1) to combine the northern portion of Planning Area 8 and the southern portion of Planning Area 12 and change the Specific Plan Land Use designation from Commercial Retail (CR) and Commercial Office (CO) to Light Industrial (LI) for APN 963-030-012, *see Figure 7: Existing Specific Plan Land Use Plan* and *Figure 8: Proposed Specific Plan Land Use Plan*. The parcel to the immediate south of the Project site (part of APN 963-030-020) shall remain as CO.
- A Change of Zone (CZ) to update the Borel Airpark Specific Plan (Specific Plan 265A1) adopted Ordinance to reflect the proposed land uses (page 4, Section f; and pages 5-6, Section i).
- A Plot Plan (PP) for the proposed Winchester Road Business Park which includes seven (7) buildings up to 430,000 square feet and associated improvements.

Proposed Site Plan:

The Project includes seven (7) buildings totaling 430,100 square feet that will be constructed in three (3) phases. Phase 1 includes the construction of Building 1 (Industrial) and Building 2 (Industrial), totaling 151,000 square feet. Phase 2 includes the construction of Building 3 (Industrial) and Building 4 (Industrial), totaling 167,800 square feet. Phase 3 includes the construction of Building 5 (Industrial), Building 6 (Multi-Tenant), and Building 7 (Multi-Tenant), totaling 111,300 square feet. The proposed phasing and square footage breakdown for each of the seven (7) buildings is provided below in *Table 1 – Proposed Site Plan Building Summary*. The Project includes 330 standard parking stalls, 115 Electric Vehicle (EV) capable parking stalls, 27 EV supply equipment parking stalls, 26 handicap parking stalls, and 103 compact parking stalls for a total of 601 parking stalls, as well as 43 dock doors. Additionally, the Project includes 3.73 acres of total landscaped area (18% of the Project site).

Table 1: Proposed Site Plan Building Summary

Phase	Building	Number of Suites	Building Footprint (SF)	Building Mezzanine (SF)	Office (SF)	Warehouse (SF)	Fabrication (SF)	Industrial (SF)	Total Building (SF)
1	1	2	58,000	8,000	16,000	50,00	-	-	66,000
	2	2	77,000	8,000	16,000	69,000	-	-	85,000
2	3	2	74,000	8,000	16,000	66,000	-	-	82,000
	4	2	77,800	8,000	16,000	69,800	-	-	85,800
3	5	4	34,300	5,000	5,000	26,800	2,500	5,000	39,300
	6	4	26,000	10,000	6,000	22,500	2,500	5,000	36,000
	7	1	26,000	10,000	6,000	22,500	2,500	5,000	36,000
<i>Total Building Area</i>			373,100	57,000	81,000	326,600	7,500	15,000	430,100

Alternative Site Plan:

The Applicant also proposes an Alternative Site Plan which includes a total of 424,800 square feet that will be constructed in three (3) phases. Phase 1 includes the construction of Building 1 (Industrial) and Building 2 (Industrial), totaling 151,000 square feet. Phase 2 includes the construction of Building 3 (Industrial) and Building 4 (Industrial), totaling 167,800 square feet. Phase 3 includes the construction of Building 5 (Multi-Tenant), Building 6 (Multi-Tenant), and Building 7 (Multi-Tenant), totaling 106,000 square feet. The proposed phasing and square footage breakdown for each of the seven (7) buildings is provided below in *Table 2 – Alternative Site Plan Building Summary*. The Project includes 362 standard parking stalls, 115 Electric Vehicle (EV) capable parking stalls, 27 EV supply equipment parking stalls, 26 handicap parking stalls, and 103 compact parking stalls for a total of 633 parking stalls, as well as 40 dock doors. Additionally, the Project includes 3.73 acres of total landscaped area (18% of the Project site).

Table 2: Alternative Site Plan Building Summary

Phase	Building	Number of Suites	Building Footprint (SF)	Building Mezzanine (SF)	Office (SF)	Warehouse (SF)	Fabrication (SF)	Industrial (SF)	Total Building (SF)
1	1	2	58,000	8,000	16,000	50,00	-	-	66,000
	2	2	77,000	8,000	16,000	69,000	-	-	85,000
2	3	2	74,000	8,000	16,000	66,000	-	-	82,000
	4	2	77,800	8,000	16,000	69,800	-	-	85,800
3	5	4	28,000	6,000	5,000	21,500	2,500	5,000	34,000
	6	4	26,000	10,000	6,000	22,500	2,500	5,000	36,000
	7	4	26,000	10,000	6,000	22,500	2,500	5,000	36,000
<i>Total Building Area</i>			366,800	58,000	81,000	321,300	7,500	15,000	424,800

Access is proposed via one (1) twenty-eight-foot (28') wide driveway on Winchester Road, one (1) twenty-eight-foot (28') wide driveway and one forty-foot (40') wide driveway on Sky Canyon Drive, and two (2) forty-foot (40') wide driveways on Sparkman Way. Street improvements include:

- Part-width improvements and Right-of-Way (ROW) dedication on Sky Canyon Drive. Sky Canyon Drive improvements will be completed with the construction of Phase 1.

- Part-width improvements and ROW dedication on Sparkman Way. Sparkman Way improvements will be completed with the construction of Phase 2.
- Half-width ROW dedication and half-width improvements on Winchester Road from Sparkman Way to the southern property line including asphalt concrete pavement, curb and gutter, sidewalk, concrete median, and landscaping. Winchester Road improvements will be completed with the construction of Phase 3.



Project Location

Figure 1: Regional Vicinity

Winchester Road Industrial Business Park
French Valley, County of Riverside

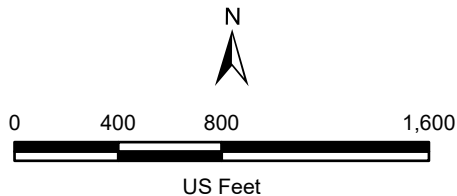
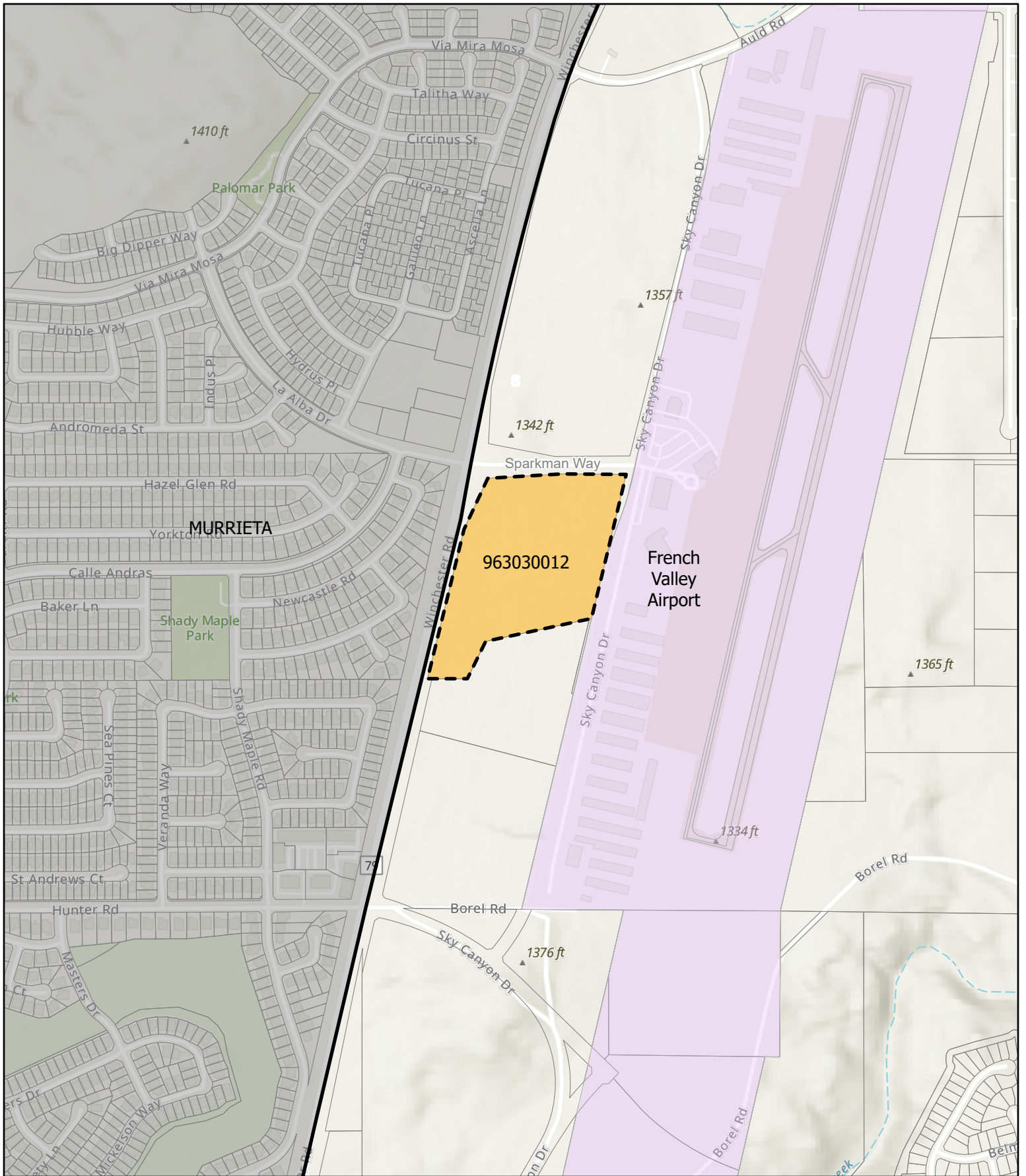


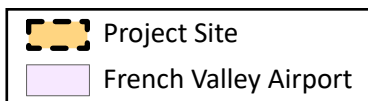
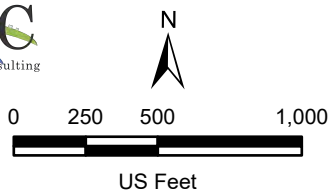
Figure 2: Assessor's Parcel Number

Winchester Road Industrial Business Park
 French Valley, County of Riverside



Figure 3: Project Site Aerial Imagery

Winchester Road Industrial Business Park
 French Valley, County of Riverside



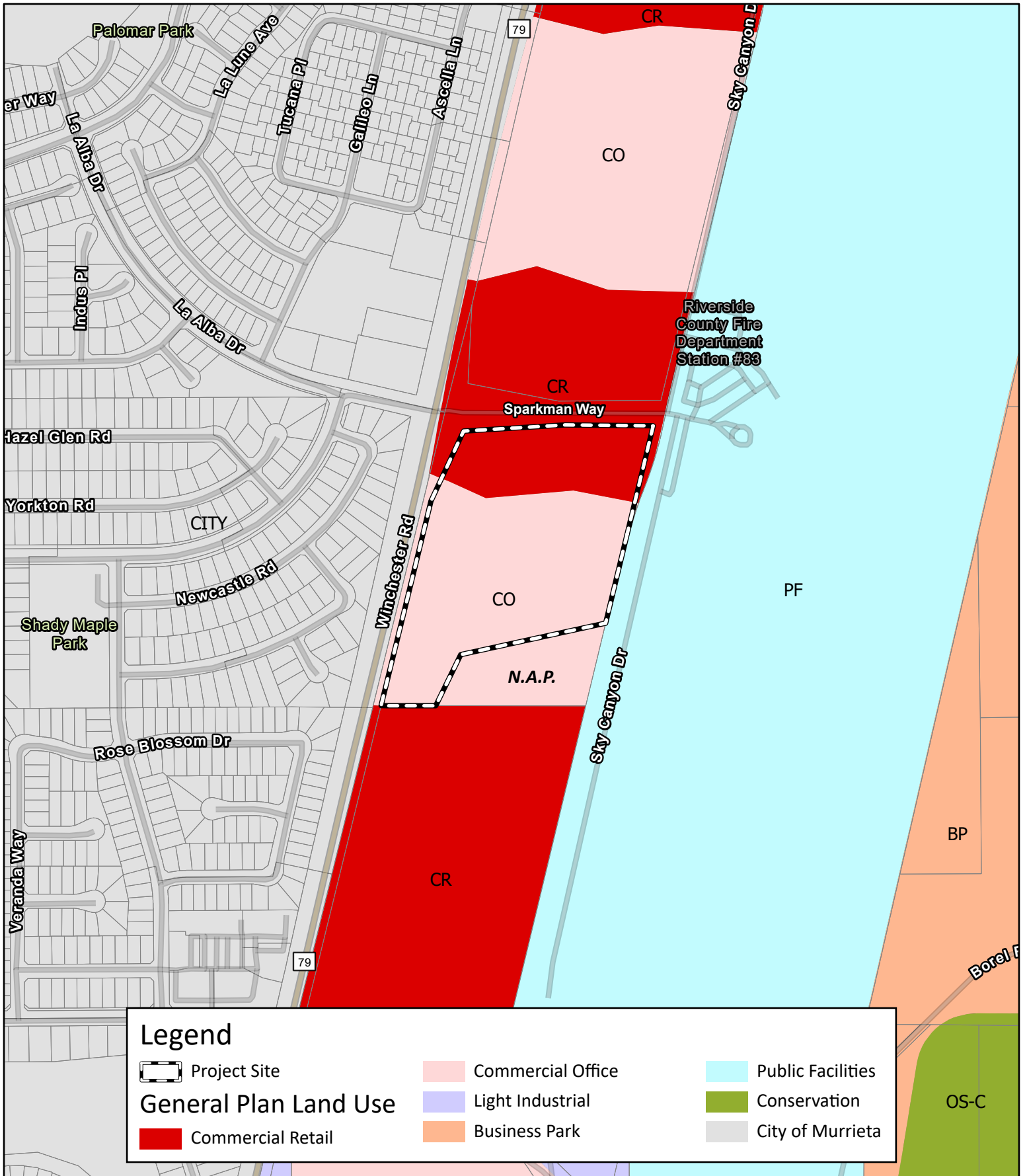


Figure 4: Existing General Plan Land Use Designation

Winchester Road Industrial Business Park
French Valley, County of Riverside



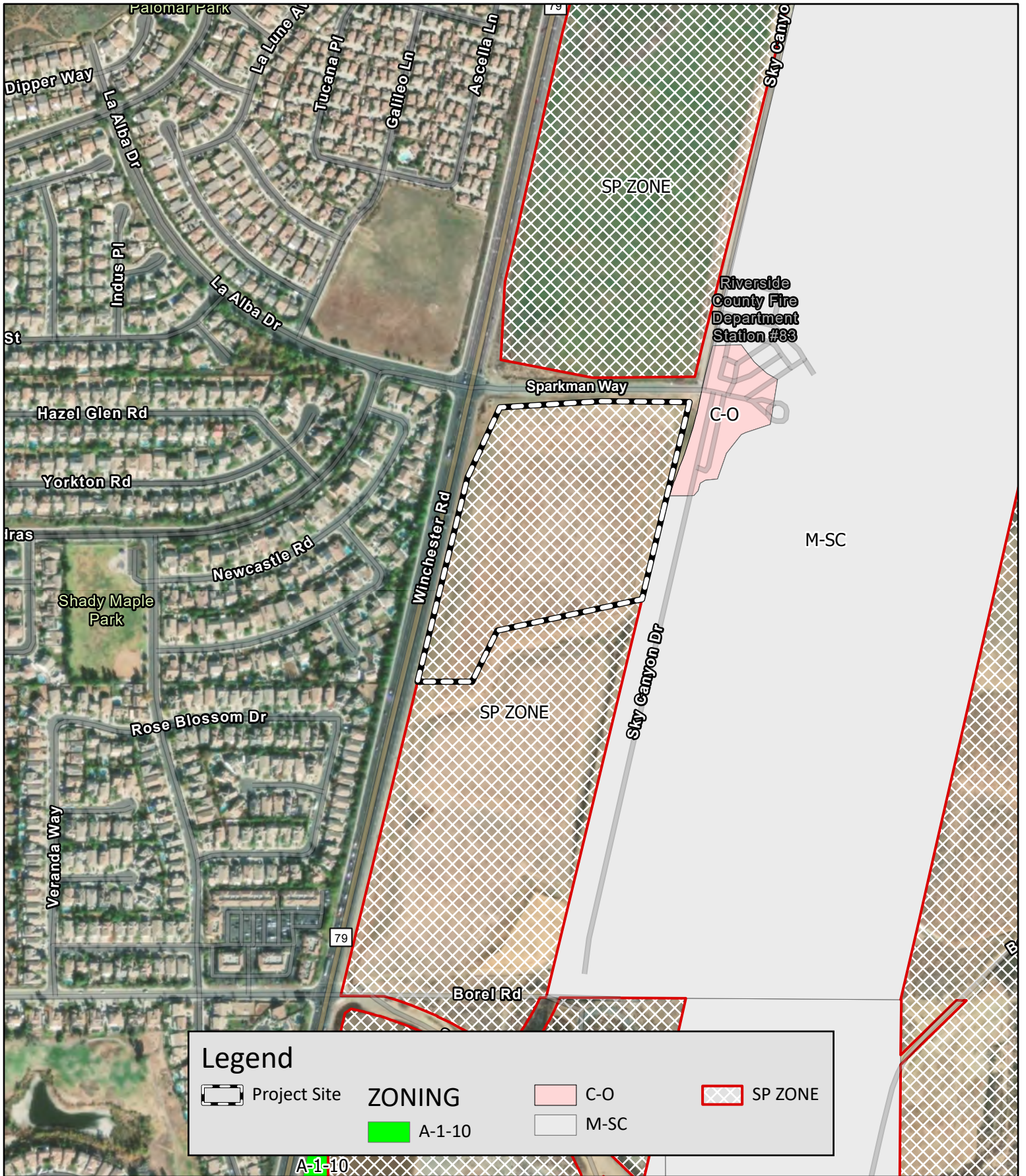
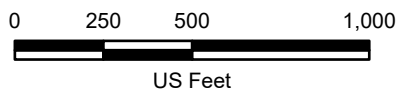


Figure 5: Zoning Designation

Winchester Road Industrial Business Park
 French Valley, County of Riverside



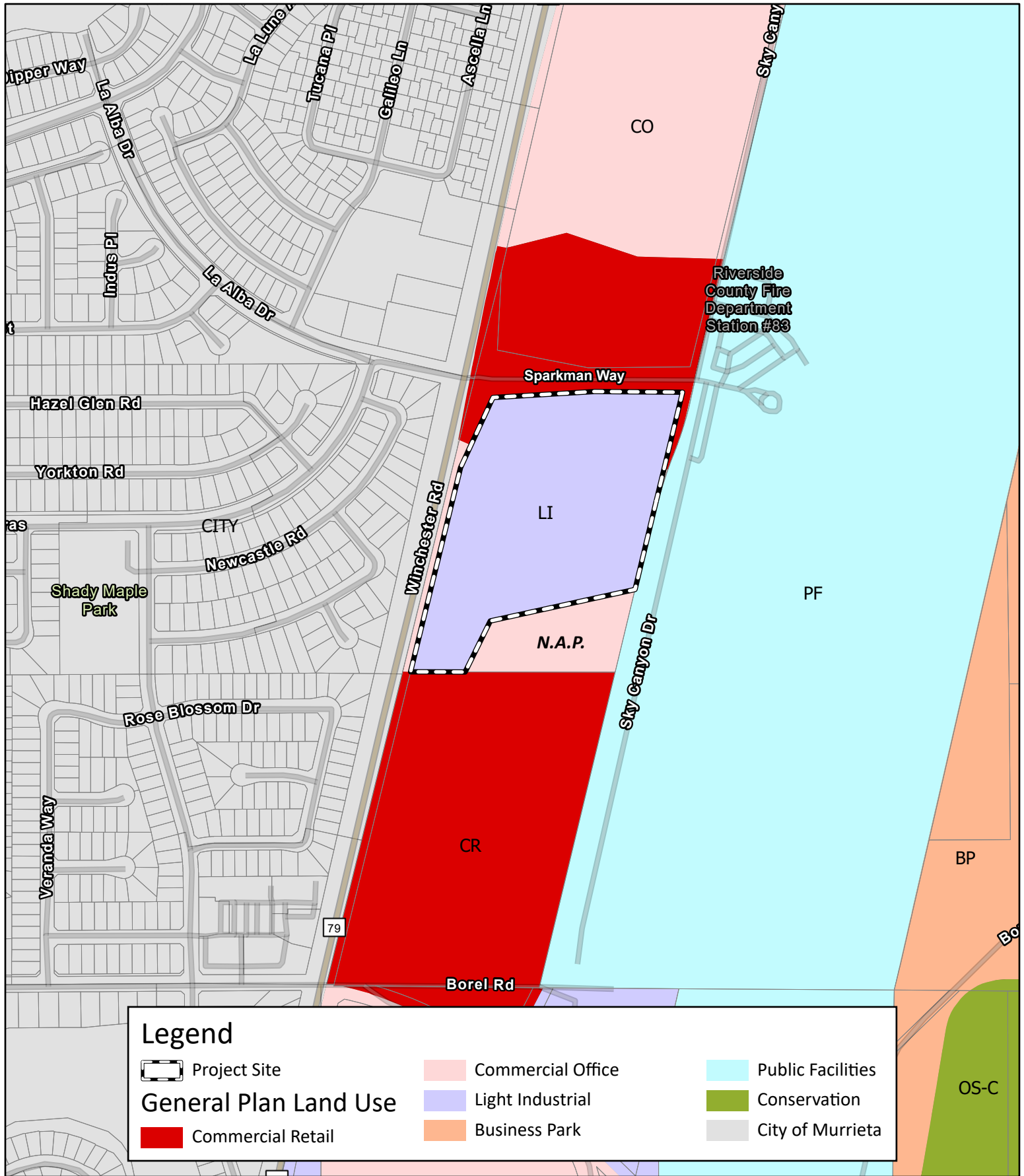
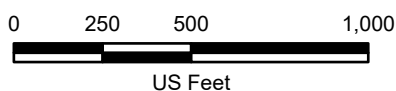
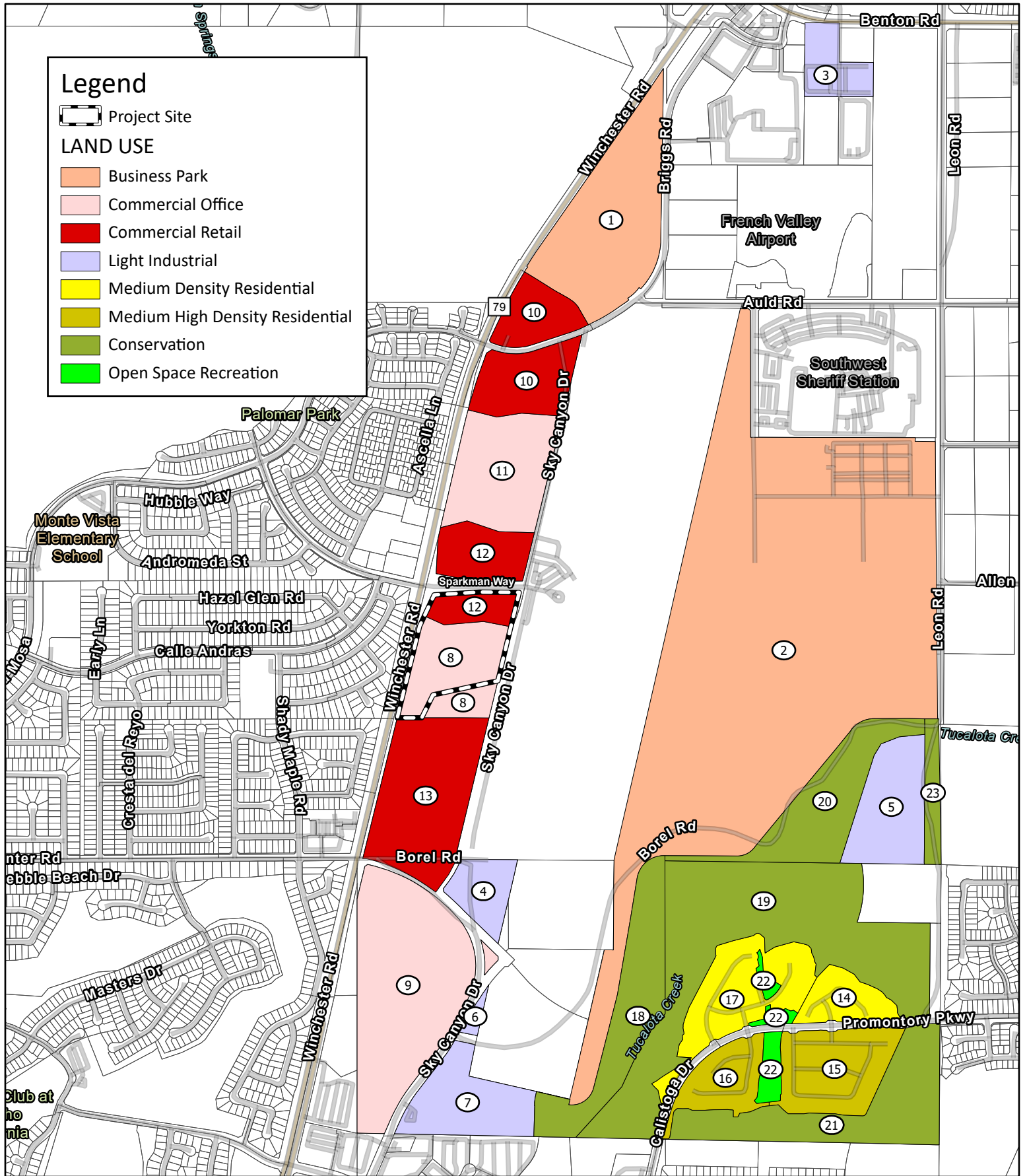


Figure 6: Proposed General Plan Amendment (with a Specific Plan)

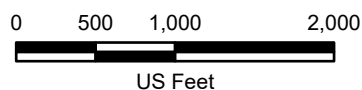
Winchester Road Industrial Business Park
French Valley, County of Riverside

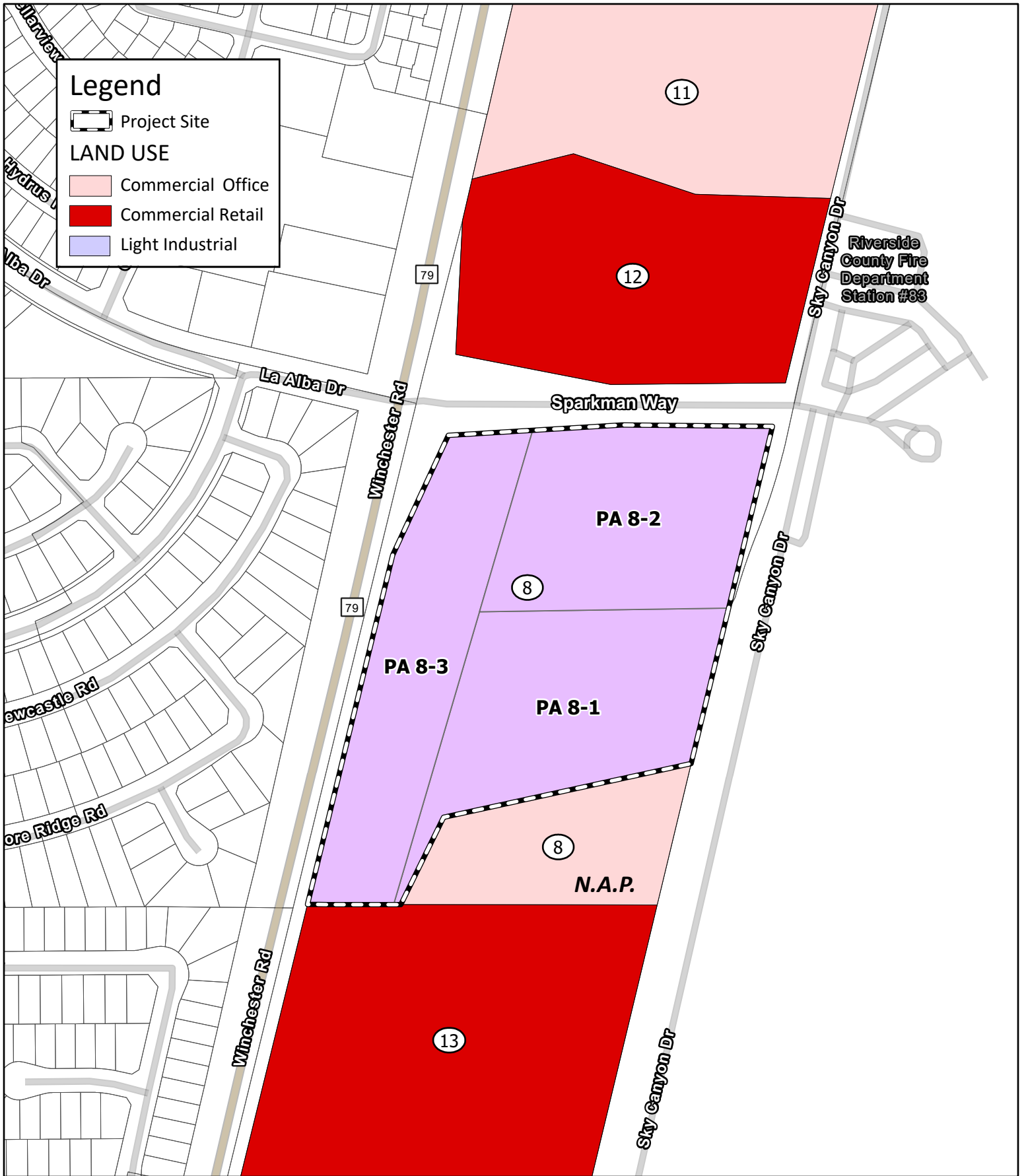




**Figure 7: Existing Specific Plan
Land Use Plan**

Winchester Road Industrial Business Park
French Valley, County of Riverside

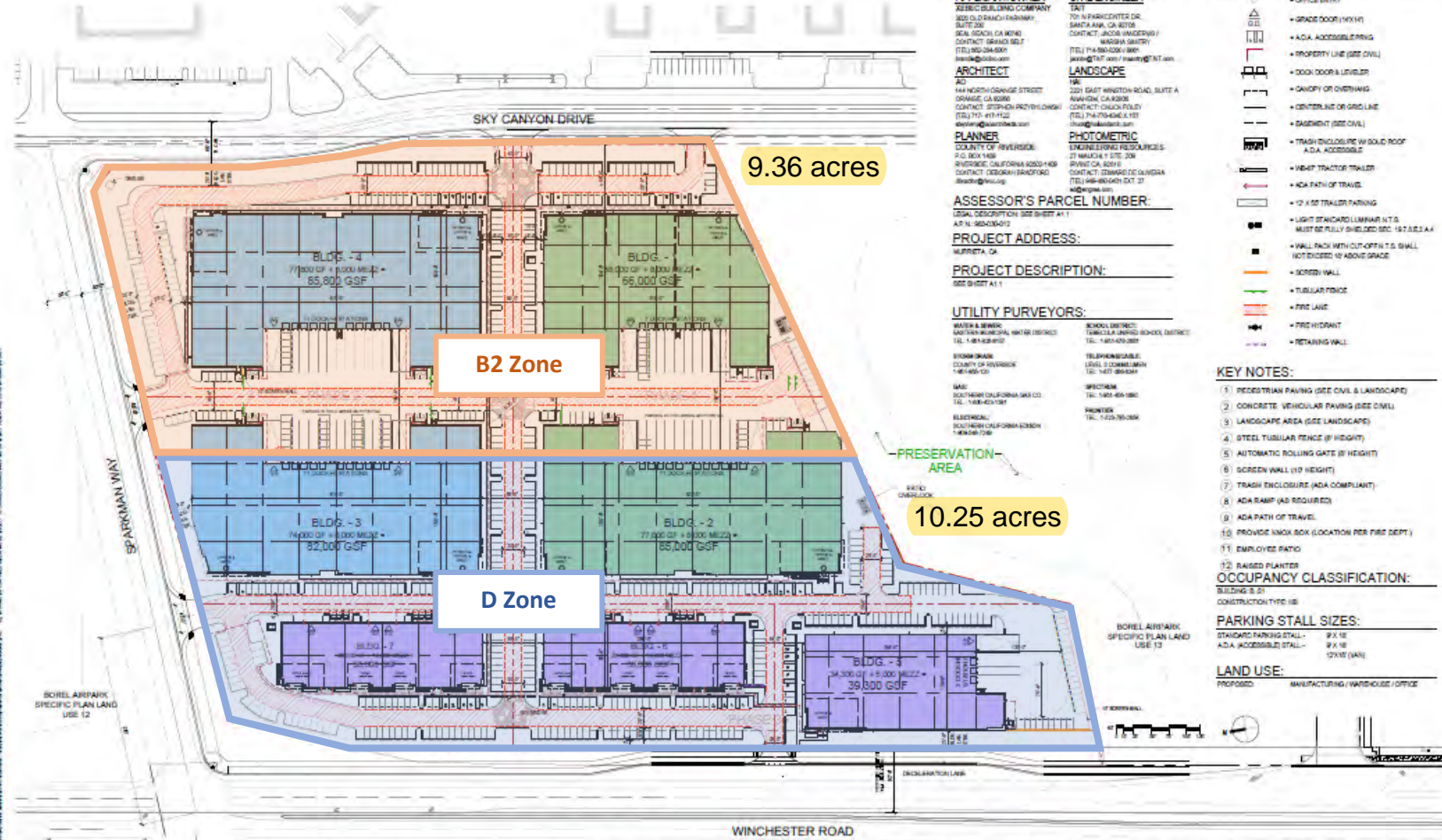




**Figure 8: Proposed Specific Plan
Land Use Plan**

Winchester Road Industrial Business Park
French Valley, County of Riverside





TEAM PLAYERS:

APPLICANT/OWNER:
 ABBEY BUILDING COMPANY
 300 D. D. BRANCH PARKWAY
 SUITE 200
 SCAL SCAL CA 94060
 CONTACT: BRAD BELL
 (TEL) 925-284-8800
 bradbell@abbey.com

ARCHITECT:
 J2
 141 WOODS GORGE STREET
 DUNSMIRE, CA 94505
 CONTACT: STEPHEN PROBY (OWNER)
 (TEL) 707-471-1122
 stephen@j2architect.com

PLANNER:
 COUNTY OF RIVERSIDE
 P.O. BOX 1408
 RIVERSIDE, CALIFORNIA 92503-1408
 CONTACT: DEBORAH SANDFORD
 dsandf@rivco.net

CIVIL ENGINEER:
 T&T
 700 N PARKCENTER DR
 SAN ANGELO, CA 92578
 CONTACT: JACOB WANDERER /
 WENDY SANDY
 (TEL) 760-258-1800
 jacob@tandt.com / wendy@tandt.com

LANDSCAPE:
 T&T
 3251 EAST WINTON ROAD, SUITE A
 ANAHEIM, CALIFORNIA
 CONTACT: CRAIG POLY
 (TEL) 714-754-6624 x101
 craig@tandt.com

PHOTOMETRIC:
 ENGINEERING RESOURCES
 27 WILSON ST. 308
 RIVINGTON, CA 92514
 CONTACT: EDWARD DE OLIVERIA
 (TEL) 951-868-0000 EXT 217
 edo@ereng.com

ASSESSOR'S PARCEL NUMBER:
 47114000000

PROJECT ADDRESS:
 MURRIETA, CA

PROJECT DESCRIPTION:
 SEE SHEET A1.1

UTILITY PURVEYORS:

WATER & SEWER:
 SOUTHERN CALIFORNIA WATER SERVICES
 TEL: 1-800-638-2822

STORM DRAIN:
 COUNTY OF RIVERSIDE
 1401455-02

GAS:
 SOUTHERN CALIFORNIA GAS CO.
 TEL: 1-800-453-5387

ELECTRICAL:
 SOUTHERN CALIFORNIA Edison
 800-453-1288

TELEPHONE/COAXIAL:
 LEVEL 3 COMMUNICATIONS
 TEL: 1-877-884-8848

TELEVISION:
 TEL: 1-800-654-5887

POWER:
 TEL: 1-800-795-2888

- GRAPHIC LEGEND:**
- OFFICE ENTRY
 - GRADE DOOR (HYD)
 - ADA ACCESSIBLE SPRING
 - PROPERTY LINE (SEE CIVIL)
 - DOOR DOOR & LEVELER
 - CANOPY OR OVERHANG
 - CENTELINE OR GROUND LINE
 - SEGMENT (SEE CIVIL)
 - TRASH ENCLOSURE (W/ SOLID ROOF A.D.A. ACCESSIBLE)
 - HEAVY TRACTOR TRAILER
 - ADA PATH OF TRAVEL
 - 12' X 30' TRAILER PARKING
 - LIGHT STANDARD LUMINAIR N.T.S. MUST BE FULLY SHIELDED SEC. 167 A.3.3 A.4
 - WALL FACE WITH C/PTN T.S. SHALL NOT EXCEED 12' ABOVE GRADE
 - SCREENED WALL
 - TUBULAR FENCE
 - FIRE LAKE
 - FIRE HYDRANT
 - RETAINING WALL

- KEY NOTES:**
- PEDESTRIAN PAVING (SEE CIVIL & LANDSCAPE)
 - CONCRETE VEHICULAR PAVING (SEE CIVIL)
 - LANDSCAPE AREA (SEE LANDSCAPE)
 - STEEL TUBULAR ROLLING GATE (BY HEIGHT)
 - AUTOMATIC ROLLING GATE (BY HEIGHT)
 - SCREENED WALL (10' HEIGHT)
 - TRASH ENCLOSURE (ADA COMPLIANT)
 - ADA RAMP (AS REQUIRED)
 - ADA PATH OF TRAVEL
 - PROVIDE KNOX BOX (LOCATION PER FIRE DEPT.)
 - EMPLOYEE RATIO
 - RAISED PLANTED
- OCCUPANCY CLASSIFICATION:**
 BUILDING 8-1
 CONSTRUCTION TYPE III-B
- PARKING STALL SIZES:**
 STANDARD PARKING STALL: 9' X 18'
 A.D.A. ACCESSIBLE STALL: 9' X 18' (MIN) 12' X 18' (MAX)
- LAND USE:**
 PROPOSED: MANUFACTURING / WAREHOUSE / OFFICE

- SHEET DESCRIPTION:**
- A0A: SITE PLAN A & PROJECT DATA
 - A1R: ALTERNATIVE SITE PLAN R
 - A1C: TRUCK CIRCULATION PLAN
 - A11: CONCEPTUAL PARKING PLAN A
 - A13: CONCEPTUAL MEASUREMENT PLAN
 - A13: CONCEPTUAL FLOOR PLAN
 - A14: CONCEPTUAL ELEVATIONS
 - A15: CONCEPTUAL COLOR ELEVATIONS
 - A22: CONCEPTUAL MEASUREMENT PLAN
 - A23: CONCEPTUAL FLOOR PLAN
 - A24: CONCEPTUAL ELEVATIONS
 - A25: CONCEPTUAL COLOR ELEVATIONS
 - A32: CONCEPTUAL MEASUREMENT PLAN
 - A33: CONCEPTUAL FLOOR PLAN
 - A34: CONCEPTUAL ELEVATIONS
 - A35: CONCEPTUAL COLOR ELEVATIONS
 - A42: CONCEPTUAL MEASUREMENT PLAN
 - A43: CONCEPTUAL FLOOR PLAN
 - A44: CONCEPTUAL ELEVATIONS
 - A45: CONCEPTUAL COLOR ELEVATIONS
 - A52: CONCEPTUAL MEASUREMENT PLAN
 - A53: CONCEPTUAL FLOOR PLAN
 - A54: CONCEPTUAL ELEVATIONS
 - A55: CONCEPTUAL COLOR ELEVATIONS
 - A62: CONCEPTUAL MEASUREMENT PLAN
 - A63: CONCEPTUAL FLOOR PLAN
 - A64: CONCEPTUAL ELEVATIONS
 - A65: CONCEPTUAL COLOR ELEVATIONS
 - A72: CONCEPTUAL MEASUREMENT PLAN
 - A73: CONCEPTUAL FLOOR PLAN
 - A74: CONCEPTUAL ELEVATIONS
 - A75: CONCEPTUAL COLOR ELEVATIONS
 - A8: WALL AND FENCE PLAN
- PROJECT DATA:**
- APR#: 955-250420
- SITE ADDRESS: WINCHESTER ROAD, MURRIETA, CA
- SITE AREA: 198,020 SF
- DEVELOPMENT STANDARDS:**
- JURISDICTION: COUNTY OF RIVERSIDE
- EXISTING ZONING: SR - COMMERCIAL RETAIL
- PROPOSED ZONING: LI - LIGHT INDUSTRIAL
- MAX. F.A.R.: 0.5
- MAX. COVERAGE: 40% AT 400 SETBACK LINE
- MAX. HEIGHT: 40' AT 400 SETBACK LINE
- BUILDING SETBACKS:**
- ADJACENT STREET OR SPECIFIC ZONES:
 FRONT: 0'
 SIDE: 0'
 REAR: 0'
- ADJACENT LOT OTHER THAN SPECIFIC ZONES ABOVE:
 FRONT: 0'
 SIDE: 0'
 REAR: 0'
- LANDSCAPE REQUIRED:** 10%
- PARKING LOT LANDSCAPE:**
- OFF-STREET PARKING: 50% (SEE SPEC. 10.1)
- STANDARD: 9' X 18' (MIN) 12' X 18' (MAX)
- ADA ACCESSIBLE: 2% (SEE SPEC. 10.1)
- STREET LIGHTING: 10' X 10' (MIN) 12' X 12' (MAX)
- STREET LIGHTING: 10' X 10' (MIN) 12' X 12' (MAX)
- STREET LIGHTING: 10' X 10' (MIN) 12' X 12' (MAX)

SITE DATA TABLE

PHASE	BUILDING	LAND AREA AC	LAND AREA SF	FAR %	BLDG FOOTPRINT	BLDG HEIGHT	TOTAL BLDG SF	OFFICE SF	FAB/LOFTED SF	INDUSTRIAL SF	WAREHOUSE SF	OFFICE PARKING	FABRICATION PARKING	INDUSTRIAL PARKING	WAREHOUSE PARKING	TOTAL PARKING	PRG. RATIO	AVG. PRG. PER 1000 SF	MAX. PRG. PER 1000 SF	MIN. PRG. PER 1000 SF	AVG. PRG. PER 1000 SF	MAX. PRG. PER 1000 SF	MIN. PRG. PER 1000 SF	AVG. PRG. PER 1000 SF	MAX. PRG. PER 1000 SF	MIN. PRG. PER 1000 SF
1	1	42.98	1,830,272	50.7%	58,000	8,000	66,000	18,000	0	0	50,000	64	0	0	25	89	1.35+1000	17	4	4	5	5	5	18.37%		
2	2	43.82	1,857,226	54.0%	77,000	8,000	85,000	18,000	0	0	66,000	64	0	0	35	99	1.19+1000	17	4	4	5	5	5	14.11%		
3	3	43.31	1,844,206	58.9%	74,000	8,000	82,000	18,000	0	0	66,000	64	0	0	33	97	1.25+1000	17	4	4	5	5	5	14.90%		
4	4	44.37	1,904,471	45.0%	77,000	8,000	85,000	18,000	0	0	66,000	64	0	0	35	99	1.19+1000	17	4	4	5	5	5	33.17%		
5	5	42.05	1,764,494	43.9%	54,300	5,000	59,300	5,000	2,500	5,000	39,800	30	5	5	13	43	1.55+1000	13	3	3	3	3	3	33.25%		
6	6	41.73	1,753,337	47.8%	35,000	10,000	45,000	36,000	8,000	2,500	5,000	33,500	24	5	5	11	45	2.00+1000	13	3	3	4	4	4	19.94%	
7	7	41.88	1,761,772	44.0%	35,000	10,000	45,000	36,000	8,000	2,500	5,000	33,500	24	5	5	11	45	2.00+1000	13	3	3	4	4	4	34.05%	
TOTAL		418.92	18,669,073	49.5%	373,100	57,000	430,100	81,000	7,500	15,000	328,800	324	15	15	143	517	1.40+1000	11.5	27	28	33	33	33	18.72%		



WINCHESTER BUSINESS PARK
 MURRIETA, CA

French Valley Airport
 Compatibility Map

A1A
 Architecture. Design. Relationships.

3/23/2025

ORDINANCE NO. 348.4997
AN ORDINANCE OF THE COUNTY OF RIVERSIDE PROVIDING FOR LAND USE
PLANNING AND ZONING REGULATIONS AND RELATED FUNCTIONS.
ARTICLE IXd C-O (COMMERCIAL-OFFICE ZONE)

ARTICLE IXd C-O (COMMERCIAL-OFFICE ZONE)

SECTION 9.71. INTENT.

The Board of Supervisors finds that there is a need in the County of Riverside for a zone classification designed to provide areas where primarily professional and administrative offices and related uses may be located. It is the intent that this zone classification ensures that such uses are well designed and landscaped to be harmonious and compatible with surrounding land uses.

SECTION 9.72. USES PERMITTED. Applicable to Planning Areas 8-1, 8-2, & 8-3

- A. The following uses are permitted provided a plot plan has been approved pursuant to the provisions of [Section 18.30](#) of this ordinance:
1. Administrative and professional offices, including but not limited to business, law, medical, dental, chiropractic, architectural, engineering, community planning, and real estate offices, in which no activity is carried on catering to retail sales and no stock of goods is maintained for sale.
 - ~~2. Art gallery, library, reading room, museum.~~
 3. Banks and financial institutions.
 4. Employment agencies.
 5. Parking lots and parking structures.
 6. Prescription pharmacy when related and incidental to a professional office building.
 7. Tourist information centers.
 8. Travel agencies.
 - ~~9. Day care centers.~~
 - ~~10. Churches, temples and other places of religious worship.~~

Amended Effective:
09-10-99 (Ord. 348.3883) repealed

10-21-99 (Ord. 348.3888)

- B. The following uses are permitted provided a conditional use permit has been approved pursuant to [Section 18.28](#) of this ordinance:
1. Clinics, including but not limited to medical, dental and chiropractic.
 2. (Deleted)

ORDINANCE NO. 348.4997
AN ORDINANCE OF THE COUNTY OF RIVERSIDE PROVIDING FOR LAND USE
PLANNING AND ZONING REGULATIONS AND RELATED FUNCTIONS.
ARTICLE IXd C-O (COMMERCIAL-OFFICE ZONE)

3. Health and exercise centers, provided all facilities are located within an enclosed building.
- ~~4. Hotels, resort hotels and motels.~~
5. Laboratories, film, dental, medical, research or testing.
- ~~6. Restaurants, not including drive-in or take-out restaurants.~~
7. Studios for professional work in or teaching of any form of fine arts, including but not limited to photography, music, drama, and dance, where no stock of goods is maintained for sale.

C. The uses listed in Subsections a. and b. do not include sex-oriented businesses.

Amended Effective:
03-01-94 (Ord. 348.3584)

D. Any use that is not specifically listed in Subsections a. or b. may be considered a permitted or conditionally permitted use provided that the Planning Director finds that the proposed use is substantially the same in character and intensity as those listed in the designated Subsections. Such a use is subject to the permit process which governs the category in which it falls.

Amended Effective:
05-05-92 (Ord. 348.3420)

03-01-94 (Ord. 348.3584)

SECTION 9.73. DEVELOPMENT STANDARDS.

The following shall be the standards of development in the C-O Zone:

- A. Lot Area. There is no minimum lot area requirement, unless specifically required by zone classification for a particular area.
- B. Setbacks.
 1. Where the front, side, or rear yard adjoins a street, the minimum setback shall be 25 feet from the right-of-way line. Where the front, side, or rear yard adjoins a lot zoned R-R, R-1, R-A, R-2, R-3, R-4, R-6, R-T, R-T-R, W-2-M, or SP with a residential use, the minimum setback shall be 25 feet from the property line.
 2. Where the front, side, or rear yard adjoins a lot with a zoning classification other than those specified in paragraph (1) above, there is no minimum setback.
 3. Setback areas may be used for driveways, parking, and landscaping.
- C. Height Requirements. The height of structures, including buildings, shall be as follows:

**ORDINANCE NO. 348.4997AN ORDINANCE OF THE COUNTY OF RIVERSIDE PROVIDING
FOR LAND USE PLANNING AND ZONING REGULATIONS AND RELATED FUNCTIONS.
ARTICLE XI M-SC ZONE (MANUFACTURING - SERVICE COMMERCIAL)**

ARTICLE XI M-SC ZONE (MANUFACTURING - SERVICE COMMERCIAL)

SECTION 11.1. INTENT.

- A. It is the intent of the Board of Supervisors in amending this article to:
1. Promote and attract industrial and manufacturing activities which will provide jobs to local residents and strengthen the County's economic base;
 2. Provide the necessary improvements to support industrial growth;
 3. Insure that new industry is compatible with uses on adjacent lands; and,
 4. Protect industrial areas from encroachment by incompatible uses that may jeopardize industry.

SECTION 11.2. USES PERMITTED. Applicable to Planning Areas 8-1, 8-2, & 8-3

- A. Agricultural uses of the soils for crops, including the grazing of not more than two mature farm animals per acre and their immature offspring.
- B. The following uses are permitted provided a plot plan is approved pursuant to the provisions of [Section 18.30](#) of this ordinance.
1. The following industrial and manufacturing uses:
 - a. Food Products:
 - 1) Meat and poultry products, not including meat packing or slaughtering.
 - 2) Dairy products, not including dairies.
 - 3) Canning and preserving fruits and vegetables.
 - 4) Grain and bakery products.
 - 5) Sugar and confectionery products.
 - 6) Nonalcoholic beverages.
 - 7) Ice.
 - b. Textile Products:
 - 1) Cotton, wood, and synthetic weaving and finishing mills.
 - 2) Wearing apparel and accessory products.

**ORDINANCE NO. 348.4997AN ORDINANCE OF THE COUNTY OF RIVERSIDE PROVIDING
FOR LAND USE PLANNING AND ZONING REGULATIONS AND RELATED FUNCTIONS.
ARTICLE XI M-SC ZONE (MANUFACTURING - SERVICE COMMERCIAL)**

- 3) Knitting mills.
 - 4) Floor covering mills.
 - 5) Yarn and thread mills.
- c. Lumber and Wood Products:
- ~~1) Saw and planing mills.~~
 - 2) Manufacture of containers and crates.
 - 3) Fabrication of wood buildings and structures.
 - ~~4) Lumber yards.~~
 - 5) Manufacture of furniture and fixtures including cabinets, partitions and similar items.
 - 6) Fabrication of manufactured housing and mobilehome.
- d. Paper Products:
- ~~1) Paper and paperboard mills.~~
 - 2) Manufacture of containers and boxes.
 - ~~3) Paper shredding.~~
 - ~~4) (Deleted)~~
 - 5) Printing and publishing of newspapers, periodicals, books, forms, cards, and similar items.
 - 6) Binding of books and other publications.
- e. Chemicals and related products:
- 1) Manufacture of organic and inorganic compounds, not including those of a hazardous nature.
 - 2) Manufacture of drugs and pharmaceuticals.
 - 3) Soaps, cleaners, and toiletries.
 - 4) Manufacture of agricultural chemicals, not including pesticides and fertilizers.
- f. Leather Products:

**ORDINANCE NO. 348.4997AN ORDINANCE OF THE COUNTY OF RIVERSIDE PROVIDING
FOR LAND USE PLANNING AND ZONING REGULATIONS AND RELATED FUNCTIONS.
ARTICLE XI M-SC ZONE (MANUFACTURING - SERVICE COMMERCIAL)**

- ~~1) Tanning and finishing of leather.~~
- 2) Manufacture of handbags, luggage, footwear, and other personal leather goods.
- g. Stone, Clay, Glass, and Concrete Products:
 - 1) Stone cutting and related activities.
 - 2) Pottery and similar items.
 - 3) Glass blowing, pressing and cutting.
 - 4) Glassware products.
 - 5) Manufacture of concrete, gypsum, plaster and mineral products.
- h. Metal Products:
 - 1) Manufacture of cans and containers.
 - 2) Cutlery, tableware, hand tools, and hardware.
 - 3) Plumbing and heating items.
 - 4) Wrought iron fabrication.
 - 5) Manufacture and assembly of fencing.
 - 6) Machine, welding, and blacksmith shops.
 - 7) Metal stamps and forged metal products.
 - 8) Fabrication of metal buildings.
 - 9) Manufacture of ordnance and firearms, not including explosives.
 - 10) Jewelry.
- i. Machinery:
 - 1) Engines, turbines, and parts.
 - 2) Farm, garden construction, and industrial machinery.
 - 3) Office and computing machines.
 - 4) Refrigeration and heating equipment.

**ORDINANCE NO. 348.4997AN ORDINANCE OF THE COUNTY OF RIVERSIDE PROVIDING
FOR LAND USE PLANNING AND ZONING REGULATIONS AND RELATED FUNCTIONS.
ARTICLE XI M-SC ZONE (MANUFACTURING - SERVICE COMMERCIAL)**

- 5) Equipment sales, rental, and storage.
- j. Electrical Equipment:
 - 1) Electrical and electronic apparatus and components.
 - 2) Appliances.
 - 3) Lighting and wiring.
 - 4) Radio, television, and communications equipment.
 - 5) Musical and recording equipment.
- k. Transportation and Related Industries:
 - 1) Vehicles, aircraft, boats and parts manufacture.
 - ~~2) Railroad equipment.~~
 - 3) Motorcycles, bicycles, and parts manufacture.
 - 4) Travel trailers and recreational vehicles manufacture.
 - 5) Draying, freighting, and trucking operations.

05-06-99 (Ord. 348.3857) Repealed

- ~~6) Railroad yards and stations.~~
- 7) Vehicle storage and impoundment.
- 8) Trailer and boat storage.
- l. Engineering and Scientific Instruments:
 - 1) Measuring devices, watches, clocks, and related items.
 - 2) Optical goods, medical instruments, supplies, and equipment and photography equipment.
- m. Industrial Uses:
 - ~~1) Cotton ginning.~~
 - 2) Public utility substations and storage yards.
 - 3) Heliports.

**ORDINANCE NO. 348.4997AN ORDINANCE OF THE COUNTY OF RIVERSIDE PROVIDING
FOR LAND USE PLANNING AND ZONING REGULATIONS AND RELATED FUNCTIONS.
ARTICLE XI M-SC ZONE (MANUFACTURING - SERVICE COMMERCIAL)**

- ~~4) Building movers storage yard.~~
 - 5) Mini warehouses.
 - 6) Warehousing and distribution.
 - 7) Communications and microwave installations.
 - 8) Cold storage plant.
 - ~~9) Contractor storage yards.~~
2. The following service and commercial uses:
- a. Banks and financial institutions.
 - b. Blueprint and duplicating services.
 - ~~c. Gasoline and diesel service stations, not including the concurrent sale of beer and wine for off-premises consumption.~~
 - d. Laboratories, film, medical, research, or testing centers.
 - e. Office equipment sales and service.
 - f. Offices, professional sales and service, including business, law, medical, dental, chiropractic, architectural, and engineering.
 - g. Parking lots and parking structures.
 - h. Restaurants and other eating establishments.
 - i. Vehicle and motorcycle repair shops.
 - j. Barber and beauty shops.
 - ~~k. Body and fender shops, and spray painting.~~
 - l. Building materials sales yard.
 - ~~m. Day care centers.~~
 - n. Health and exercise centers.
 - o. Hardware and home improvement center.
 - ~~p. Mobilehomes, provided they are kept mobile and licensed pursuant to State law, when used for: sales offices on mobilehome sales lots; construction offices~~

ORDINANCE NO. 348.4997AN ORDINANCE OF THE COUNTY OF RIVERSIDE PROVIDING
FOR LAND USE PLANNING AND ZONING REGULATIONS AND RELATED FUNCTIONS.
ARTICLE XI M-SC ZONE (MANUFACTURING - SERVICE COMMERCIAL)

~~and caretakers quarters on construction sites for the duration of a valid building permit; agricultural worker employment offices for a maximum of 90 days in any calendar year; caretaker's quarters and office, in lieu of any other one-family dwelling located on the same parcel as a permitted industrial use.~~

- ~~q. One family dwellings on the same parcel as the industrial or commercial use provided such dwellings are occupied exclusively by the proprietor or caretaker of the use and their immediate family.~~
- r. Nurseries and garden supply stores.
- ~~s. Car and truck washes.~~
- t. Signs, on-site advertising.
- ~~u. Feed and grain sales.~~
- v. Truck and trailer sales and rental.
- ~~w. Fortune telling, spiritualism, or similar activity.~~
- ~~x. Mobilehome sales lots.~~
- y. Recycling collection facilities.
- ~~z. Churches, temples, or other structures used primarily for religious worship.~~

Amended Effective:
11-30-95 (Ord. 348.3753)

C. The following uses are permitted provided a conditional use permit has been granted pursuant to [Section 18.28](#) of this ordinance:

1. Meat packing plants, not including slaughtering or rendering of animals.
- ~~2. Cemeteries, crematories, and mausoleums.~~
- ~~3. Paper storage and recycling, not within a building.~~
4. Brewery, distillery, or winery.
5. Acid and abrasives manufacturing.
- ~~6. Fertilizer production, organic or inorganic.~~
- ~~7. Petroleum and bulk fuel storage, above ground, pursuant to County Ordinance No. 546.~~
- ~~8. Paints and varnishes manufacturing and incidental storage.~~

**ORDINANCE NO. 348.4997AN ORDINANCE OF THE COUNTY OF RIVERSIDE PROVIDING
FOR LAND USE PLANNING AND ZONING REGULATIONS AND RELATED FUNCTIONS.
ARTICLE XI M-SC ZONE (MANUFACTURING - SERVICE COMMERCIAL)**

- ~~9. Concrete batch plants and asphalt plants.~~
- 10. Recycling processing facilities.
- ~~11. (Deleted)~~
- 12. Airports.
- ~~13. Poultry and egg processing.~~
- 14. Recycling of wood, metal, and construction wastes.
- ~~15. Natural gas storage, above ground.~~
- ~~16. Drive in theaters.~~
- ~~17. Disposal service operations, not including transfer stations.~~
- 18. Draying, freighting and trucking operations.
- 19. Solar power plant on a lot 10 acres or larger.
- 20. Parolee-Probationer Home developed in accordance with the standards set forth in [Section 18.52](#) of this ordinance.

Added Effective:

05-06-99 (Ord. 348.3857)

Ord. 348.4705 Item 16.2 of 11/08/11 (Effective Date 12/08/11)

Ord. 348.4744 Item 16.1 of 06/19/12 (Effective Date 07/19/12)

D. Any mining operation that is subject to the California Surface Mining and Reclamation Act of 1975 provided a valid surface mining permit has been granted pursuant to County Ordinance No. 555.

~~E. (Deleted)~~

Amended Effective:

Ordinance No. 348.4911 Item 21.1 of 09.10.19 (Effective

Date: 10.10.19)

F. Sex-oriented businesses, subject to the provisions of County Ordinance No. 743. The uses listed in [Subsections A, B, and C](#) do not include sex-oriented businesses.

Amended Effective:

03-01-94 (Ord. 348.3584)

G. Any use that is not specifically listed in [Subsections B](#) and [C](#) may be considered a permitted or conditionally permitted use provided that the Planning Director finds that the proposed use is substantially the same in character and intensity as those listed in the designated Subsections. Such a use is subject to the permit process which governs the category in which it falls.

**ORDINANCE NO. 348.4997AN ORDINANCE OF THE COUNTY OF RIVERSIDE PROVIDING
FOR LAND USE PLANNING AND ZONING REGULATIONS AND RELATED FUNCTIONS.
ARTICLE XI M-SC ZONE (MANUFACTURING - SERVICE COMMERCIAL)**

- H. Industrial Hemp Activities are permitted or conditionally permitted in subsections A., B., or C. in Section 11.2 pursuant to the provisions set forth in Article XIXm of this ordinance including, but not limited to, permit processing, location, standards and approval requirements.

Amended Effective:

01-29-85 (Ord. 348.2443)
07-16-85 (Ord. 348.2496)
04-04-87 (Ord. 348.2669)
05-04-89 (Ord. 348.3023)
06-20-89 (Ord. 348.3043)
07-11-89 (Ord. 348.3047)

03-01-94 (Ord. 348.3584)
11-30-95 (Ord. 348.3753)
05-06-99 (Ord. 348.3857)
Ord. 348.4931 Item 21.1 of 11/10/2020 (Effective Date
12/10/2020)

SECTION 11.3. PLANNED INDUSTRIAL DEVELOPMENTS.

Planned industrial developments are permitted provided a land division has been approved pursuant to Riverside County Ordinance No. 460.

SECTION 11.4. DEVELOPMENT STANDARDS.

The following development standards shall apply in the M-SC Zone.

- A. Lot Size. The minimum lot size shall be 10,000 square feet with a minimum average width of 75 feet, except that a lot size not less than 7,000 square feet and an average width of not less than 65 feet may be permitted when sewers are available and will be utilized for the development.
- B. Setbacks.
1. Where the front, side, or rear yard adjoins a lot zoned R-R, R-1, R-A, R-2, R-3, R-4, R-6, R-T, R-T-R, or W-2-M, the minimum setback shall be 25 feet from the property line.
 2. Where the front, side, or rear yard adjoins a lot with zoning classification other than those specified in paragraph (1) above, there is no minimum setback.
 3. Where the front, side, or rear yard adjoins a street, the minimum setback shall be 25 feet from the property line.
 4. Within the exception of those portions of the setback area for which landscaping is required by [Subsection E.](#) below, the setback area may only be used for driveways, automobile parking, or landscaping. A setback area which adjoins a street separating it from a lot with a zoning classification other than those zones specified in paragraph (1) above, may also be used for loading docks.
- C. Height Requirements. The height of structures, including buildings, shall be as follows:

ORDINANCE 348.XXXX. ADDITIONAL USES PERMITTED IN PLANNING AREAS 8-1, 8-2, & 8-3.

1. Data centers
2. Parcel delivery services
3. Self-storage facilities

ORDINANCE 348.XXXX. ADDITIONAL USES PERMITTED IN PLANNING AREA 8-3 ONLY.

1. Dance studios
2. Employment agencies
3. Retail stores
4. Sports training studios
5. Travel agencies
6. Veterinarian offices

ORDINANCE 348.XXXX. ADDITIONAL USES PERMITTED IN PLANNING AREAS 8-1, 8-2, & 8-3 PROVIDED A CONDITIONAL USE PERMIT HAS BEEN GRANTED.

1. Aircraft factories and manufacture of parts
2. Bottle making facilities
3. Generator, manufacture of electric
4. Industrial laundry facilities
5. Laboratory testing, experimental film, motion picture
6. Manufacture and assembly of automobile and automobile parts
7. Manufacture and assembly of electric appliances
8. Manufacture and assembly of furniture
9. Manufacture and packaging of pharmaceuticals
10. Manufacture and packaging of plastic injection molding
11. Manufacture of bags
12. Manufacture of clothing
13. Manufacture of drugs

14. Manufacture of glass
15. Manufacture of health and beauty aids
16. Manufacture of heating and air conditioning equipment
17. Manufacture of lighting fixtures
18. Manufacture of medicine, patent
19. Manufacture of medical equipment
20. Manufacture of paper products
21. Manufacture of sash and door
22. Manufacture, processing, bottling, and packaging of food and beverages
23. Motion picture, television, video and other media production

NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. **Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org.** The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. For more information please contact **ALUC Planner Jackie Vega at (951) 955-0982.**

The County of Riverside Planning Department should be contacted on non-ALUC issues. For more information please contact County of Riverside Planner Kim Zuppiger at (951) 955-3200.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to javega@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please contact Barbara Santos at (951) 955-5132.

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside California

DATE OF HEARING: October 12, 2023

TIME OF HEARING: 9:30 A.M.

CASE DESCRIPTION:

ZAP1128FV23 – Xebec Building Company (Representative: Casc Engineering & Consulting, Inc.) – County of Riverside Case No. GPA23-0004 (General Plan Amendment), SP00265A02 (Specific Plan Amendment), CZ230012 (Change of Zone), PPT23-0002 (Plot Plan). A proposal to construct five industrial buildings totaling 347,100 square feet with mezzanines and two commercial buildings totaling 64,000 square feet with mezzanines on a 19.61 acres parcel, located northerly of KTM way, southerly of Sparkman Way, easterly of Winchester Road, and westerly of Sky Canyon Drive. The applicant also proposes amending the General Plan to change the sites land use designation from Commercial Retail and Commercial Office to Light Industrial. The applicant also proposes amending the sites specific plan land use designation from Commercial Retail and Commercial office to Light Industrial. Lastly, the applicant proposes changing the sites zoning to update the Borel Airpark Specific Plan adopted ordinance to reflect the proposed land uses. (Airport Compatibility Zone B2 and D of the French Valley Airport Influence Area).



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC STAFF ONLY

ALUC Case Number: ZAP1128FV23

Date Submitted: 6/13/23

AIA: French Valley

Zone: B2 & D

Public Hearing

Staff Review

Applicant

Applicant Full Name: Xebec Building Company - Attn: Brandi Self

Applicant Address: 3020 Old Ranch Parkway, Suite 200, Seal Beach, CA 90740

Phone: 562.284.5001 Email: brandis@xbcinc.com

Representative/ Property Owner Contact Information

Representative: Casc Engineering & Consulting, Inc.

Email: fcoyle@cascinc.com

Attn: Frank Coyle

Phone: 909.362.7645

Address: 1470 E. Cooley Drive, Colton, CA 92324

Property Owner: Winchester Holdings, LLC

Email: wwang@foxluggage.com

Attn: Wayne Wang

Phone: 909.767.5666

Address: 221 N. Orange Avenue, City of Industry, CA 91744

Local Jurisdiction Agency

Agency Name: County of Riverside Planning Department

Phone: 951.955.6646

Staff Contact: Deborah Bradford

Email: dbradford@rivco.org

Address: 4080 Lemon Street, 12th Floor, Riverside, CA 92501

Local Agency Case No.: PAR 220057; OAPL2300407

Project Location

Street Address: SE Corner of Sparkman Way and Winchester Road (Highway 79) Gross Parcel Size: 23.1 acres

Assessor's Parcel No.: 963-030-012

Solar

Is the project proposing solar Panels? Yes

No

If yes, please provide solar glare study.
(only if in Zone C or higher)

Data

Site Elevation:(above mean sea level) Approx. 1,325

Height of Building or structures: 41' to 50'

What type of drainage basins are being proposed and the square footage: ADS StormTech Drainage Systems; Basin A1 is 72,669 CF, Basin B1 is 31,624 CF, Basin B2 is 6,762 CF

Notice

A. NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. REVIEW TIME: Estimated time for “staff level review” is approximately 30 days from date of submittal. Estimated time for “commission level review” is approximately 45 days from date of a complete application submittal to the next available commission hearing meeting.

C. SUBMISSION PACKAGE:

Please submit all application items DIGITALLY via USB or CD:

- Completed ALUC Application Form
- Plans Package: site plans, floor plans, building elevations, grading plans, subdivision maps
- Exhibits of change of zone, general plan amendment, specific plan amendment
- Project description of existing and proposed use

Additionally, please provide:

- ALUC fee payment (Checks made out to Riverside County ALUC)
- Gummed address labels of all surrounding property owners within a 300-foot radius of project site. (Only required if the project is scheduled for a public hearing).

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

STAFF REPORT

ADMINISTRATIVE ITEMS

5.1 Director's Approvals

- A. During the period of August 16, 2023, through September 15, 2023, as authorized pursuant to Section 1.5.2(d) of the 2004 Riverside County Airport Land Use Compatibility Plan, ALUC Director Paul Rull reviewed two non-legislative case and issued a determination of consistency.

ZAP1582MA23 (Zone D) pertains to City of Perris Case No. DPR22-00030 (Development Plan Review), a proposal to revise ZAP1570MA23 removing a portion of the detention basin and adding 45 truck trailer spaces, located northerly of Ethanac Road, westerly of Sherman Road, and easterly of Trumble Road. The site is located within Airport Compatibility Zone D of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone D of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, non-residential intensity is not restricted. Although the project is located within the March Air Reserve Base/Inland Port Airport Influence Area, the nearest runway is actually Runway 15-33 at Perris Valley Airport. The elevation of Runway 15-33 at Perris Valley Airport is approximately 1,413 feet above mean sea level (AMSL) at its southerly terminus. At a distance of 10,854 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with an elevation at top of roof exceeding 1,522 feet AMSL. The site elevation is 1,433 feet AMSL, and the proposed building height is 52 feet, resulting in a top point elevation of 1,485 feet AMSL. Therefore, FAA OES review for height/elevation was not required.

ALUC Director Paul Rull issued a determination of consistency for this project on August 22, 2023.

ZAP1583MA23 (Zone E) pertains to County of Riverside Case No. TPM 38358 (Tentative Parcel Map), a proposal to divide 10.20 acres into two residential lots, located southerly of Elis Avenue, northerly of Floral Avenue, easterly of Belita Drive, and westerly of Palms Street. The project is located within Compatibility Zone E of March Air Reserve Base/Inland Port Airport Influence Area, where Zone E does not restrict residential density. Although the project is located within the March Air Reserve Base/Inland Port Airport Influence Area, the nearest runway is actually Runway 15-33 at Perris Valley Airport. The elevation of Runway 15-33 at Perris Valley Airport is approximately 1,417 feet above mean sea level (AMSL) at its northerly terminus. At a distance of 20,328 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with an elevation at top of roof exceeding 1,620 feet AMSL. The site elevation is 1,454 feet AMSL, and the proposed building height is 22 feet, resulting in a top point elevation of 1,476 feet AMSL. Therefore, FAA OES review for height/elevation was not required.

ALUC Director Paul Rull issued a determination of consistency for this project on September 14, 2023.

- 5.2 Update March Air Reserve Base Compatibility Use Study (CUS)
Presentation by Project Director Simon Housman or his designee.

X:\ALUC Administrative Items\Admin. 2023\Admin Item 10-14-23.doc

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



August 22, 2023

CHAIR

Steve Manos
Lake Elsinore

VICE CHAIR

Russell Betts
Desert Hot Springs

COMMISSIONERS

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Riverside

Steven Stewart
Palm Springs

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Moreno Valley

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STAFF

Director
Paul Rull

Simon A. Housman
Jackie Vega
Barbara Santos

County Administrative Center
4080 Lemon St., 14th Floor.
Riverside, CA 92501
(951) 955-5132

www.rcaluc.org

Nathan Perez, Project Planner
City of Perris Planning Department
135 N. D Street
Perris CA 92570-1906

RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW – DIRECTOR’S DETERMINATION

File No.: ZAP1582MA23
Related File No.: DPR22-00030 (Development Plan Review)
APN: 329-240-016 to 329-240-020, 329-240-023 to 329-240-027
Airport Zone: Zone D

Dear Mr. Perez,

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed City of Perris Case No. DPR22-00030 (Development Plan Review), a proposal to revise ZAP1570MA23 removing a portion of the detention basin and adding 45 truck trailer spaces, located northerly of Ethanac Road, westerly of Sherman Road, and easterly of Trumble Road.

The site is located within Airport Compatibility Zone D of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone D of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, non-residential intensity is not restricted.

Although the project is located within the March Air Reserve Base/Inland Port Airport Influence Area, the nearest runway is actually Runway 15-33 at Perris Valley Airport. The elevation of Runway 15-33 at Perris Valley Airport is approximately 1,413 feet above mean sea level (AMSL) at its southerly terminus. At a distance of 10,854 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with an elevation at top of roof exceeding 1,522 feet AMSL. The site elevation is 1,433 feet AMSL, and the proposed building height is 52 feet, resulting in a top point elevation of 1,485 feet AMSL. Therefore, FAA OES review for height/elevation was not required.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, provided that the City of Perris applies the following recommended conditions:

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:

- (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight or circling climb following takeoff or toward an aircraft engaged in a straight or circling final approach toward a landing at an airport, other than a DoD or FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight or circling climb following takeoff or towards an aircraft engaged in a straight or circling final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor, or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Hazards to flight
3. The attached notice shall be provided to all prospective purchasers of the property and tenants of the building, and shall be recorded as a deed notice.
4. Any proposed detention basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the detention basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

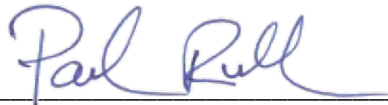
5. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers,

access gates, etc.

6. The project has been evaluated to revise ZAP1570MA23 removing a portion of the detention basin and adding 45 truck trailer spaces, change in use to any higher intensity use, change in building location, or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.

If you have any questions, please contact me at (951) 955-6893.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Paul Rull, ALUC Director

Attachments: Notice of Airport in Vicinity

cc: Hillwood (applicant/property owner)
Albert A. Webb & Associates (representative)
Gary Gosliga, March Inland Port Airport Authority
Major. David Shaw, Base Civil Engineer, March Air Reserve Base
ALUC Case File

X:\AIRPORT CASE FILES\March\ZAP1582MA23\ZAP1582MA23.LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

**THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS**

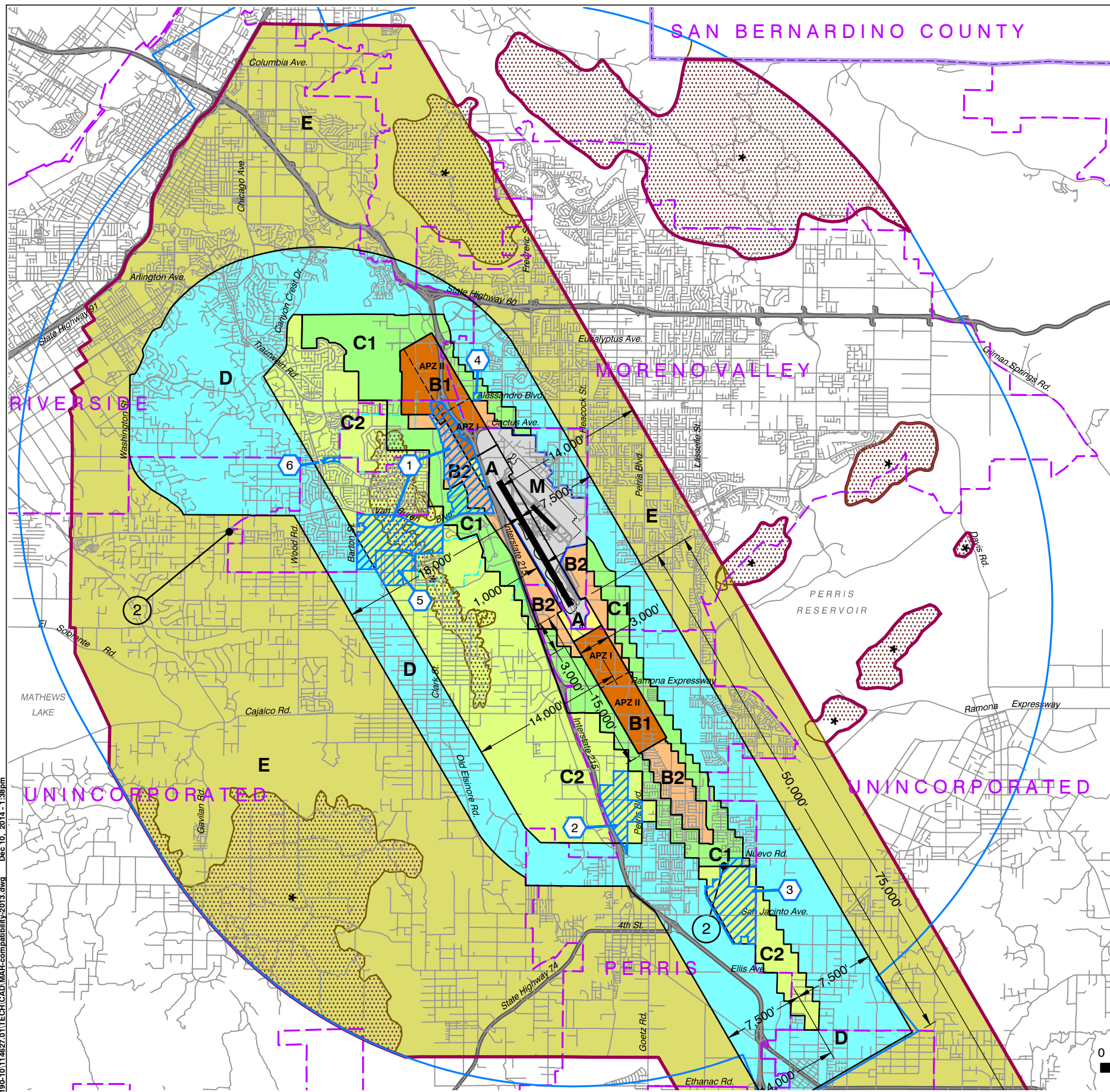
**PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES**



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



LEGEND

Compatibility Zones

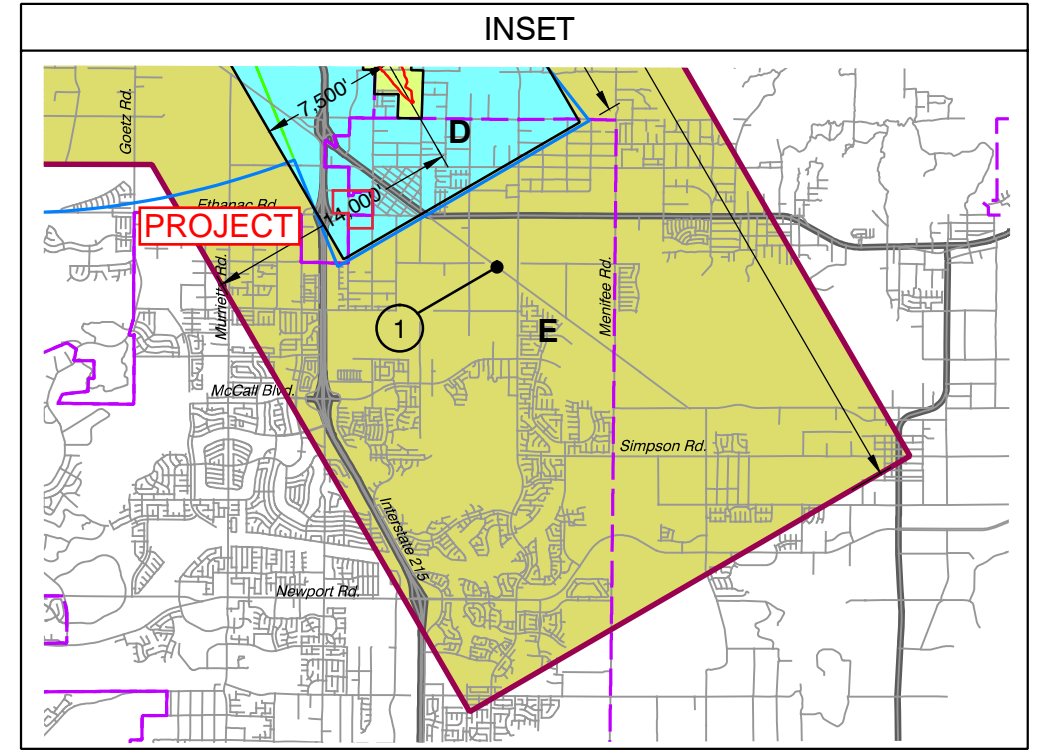
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

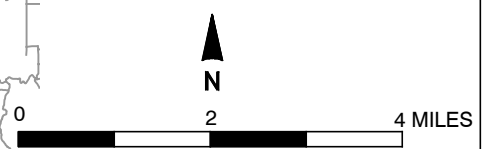
- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



**Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)**

Note:
All dimensions are measured from runway ends and centerlines.



Base map source: County of Riverside 2013

SEE INSET AT RIGHT

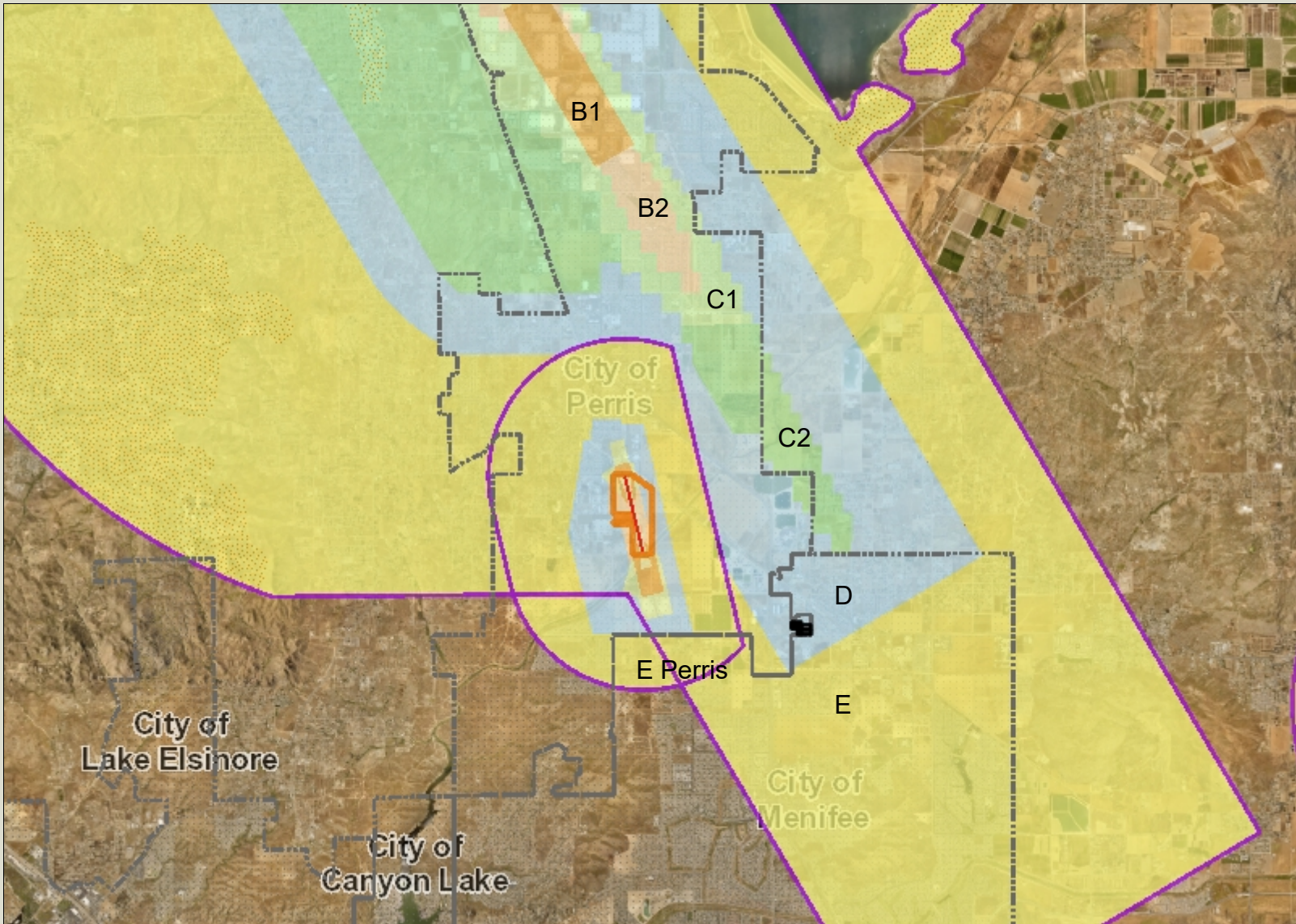
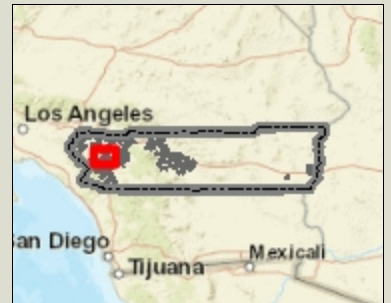
X:\18190-10\114627\01\TECH\CAD\MAR-compatibility\2013.dwg Dec 10, 2014 - 1:38pm

Prepared by Mead & Hunt, Inc. (June 2013)

Map MA-1

**Compatibility Map
March Air Reserve Base / Inland Port Airport**

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

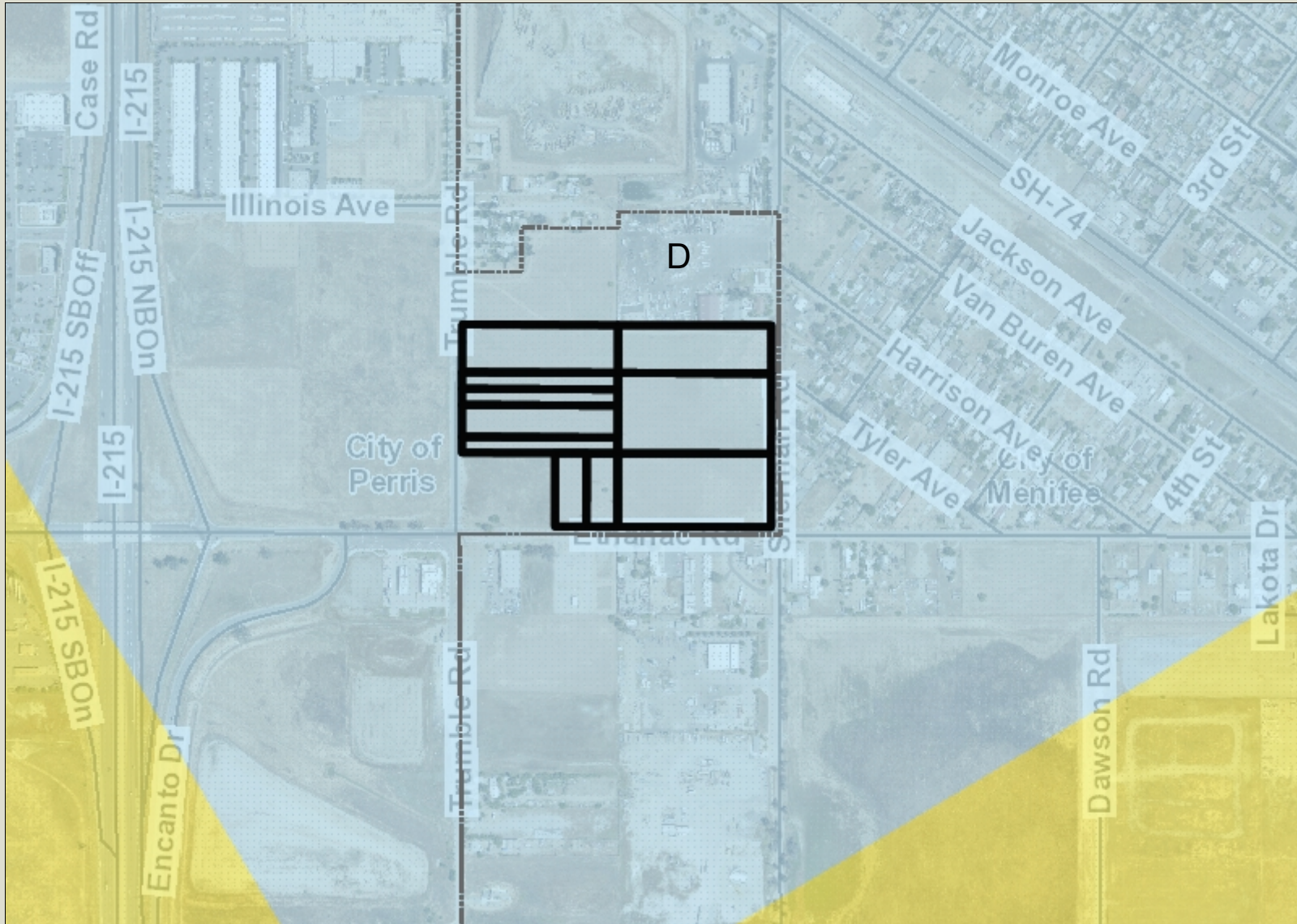


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Notes

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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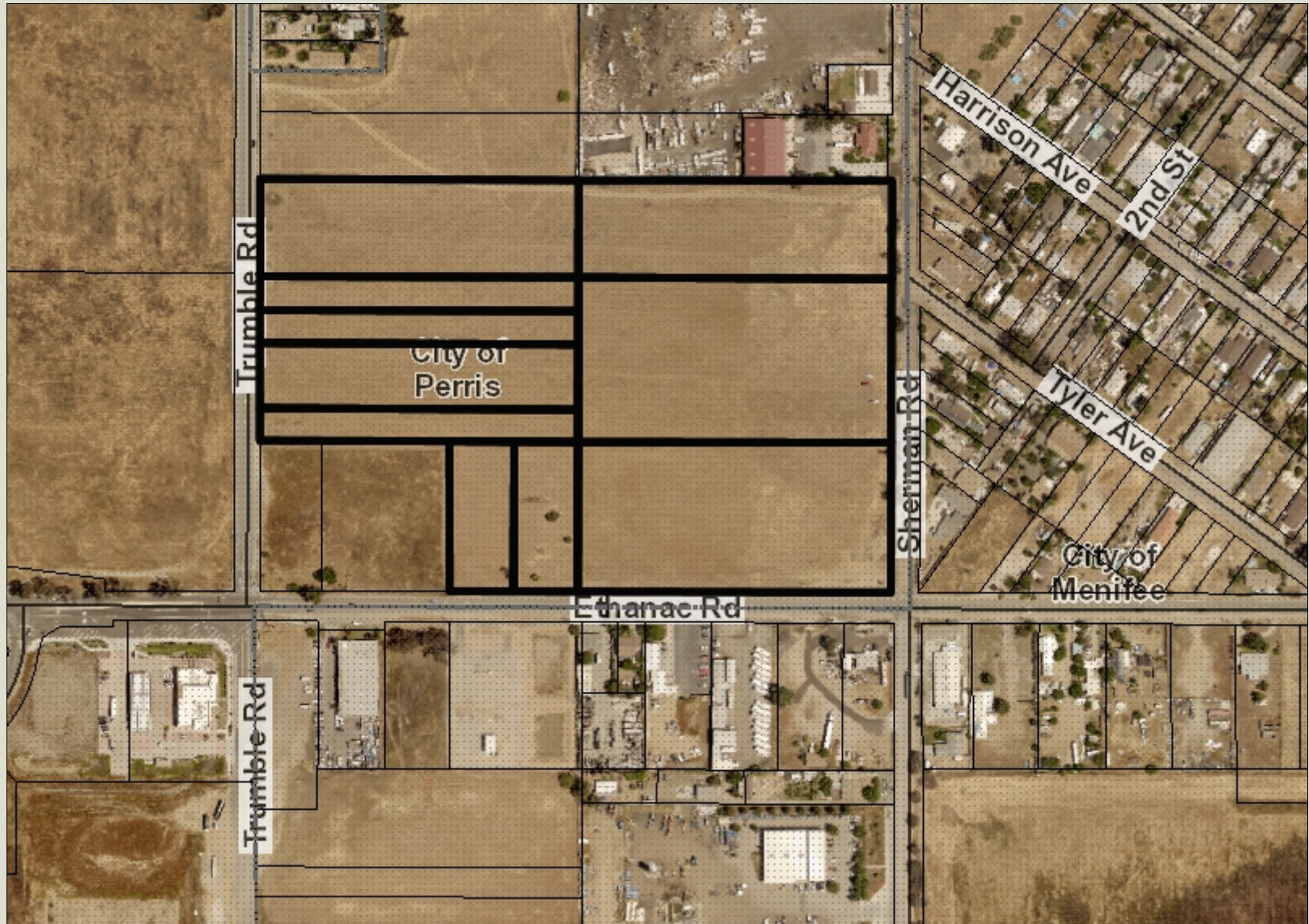
0 770 1,539 Feet

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Notes

Map My County Map



Legend

-  Parcels
-  County Centerline Names
-  County Centerlines
-  Blueline Streams
-  City Areas
-  World Street Map



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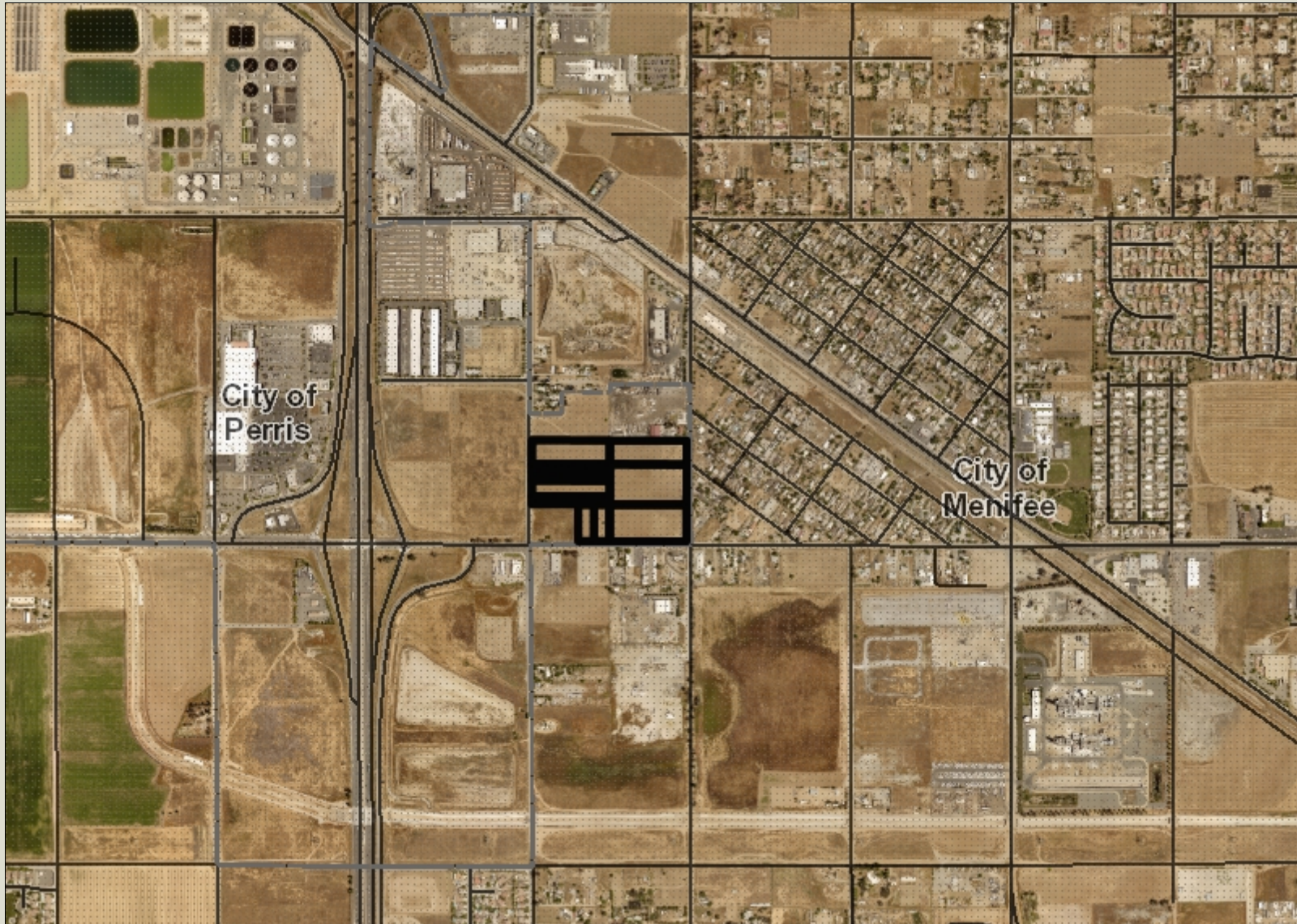


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Notes

Map My County Map



Legend

- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



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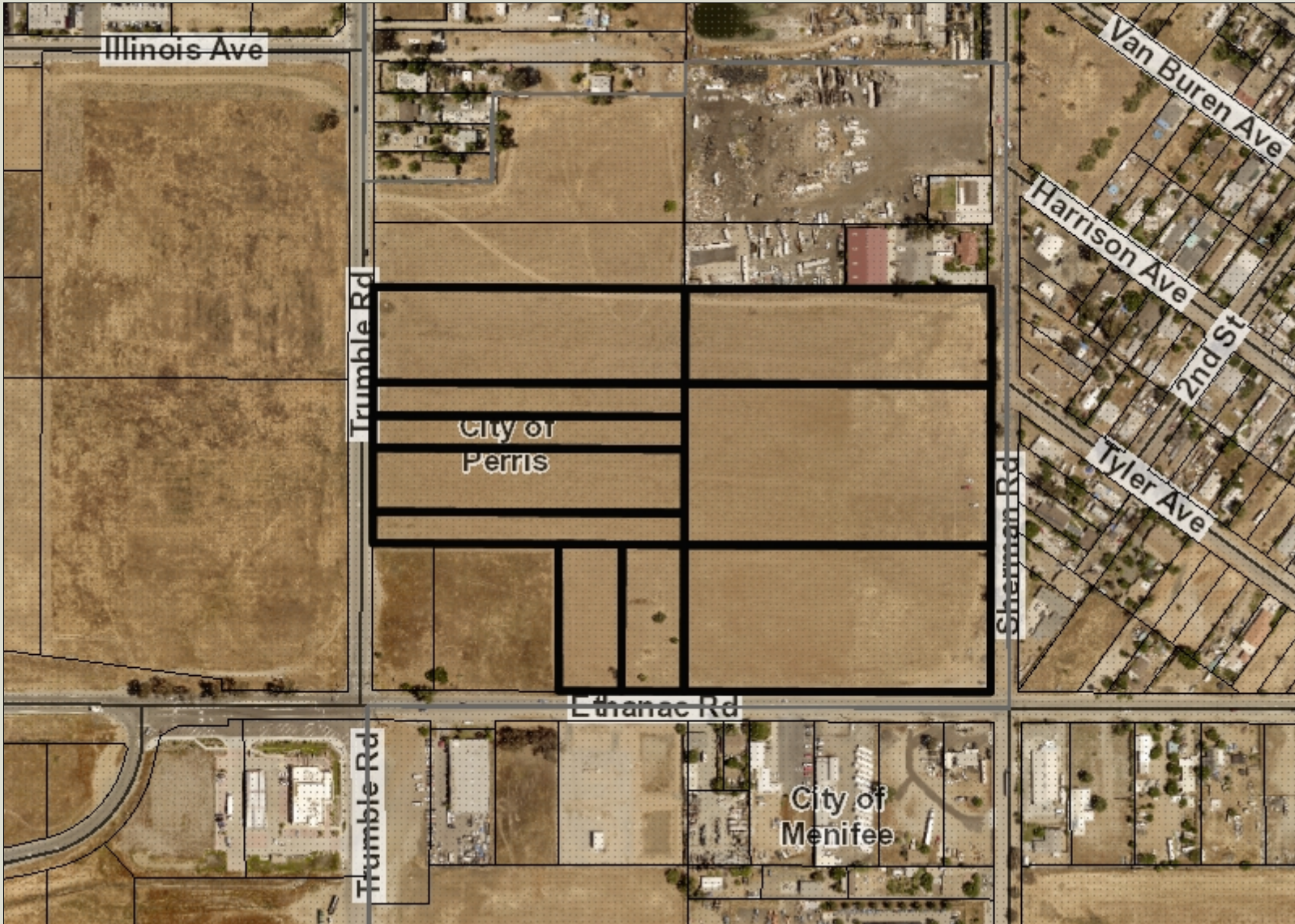
Notes



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Map My County Map



Legend

-  Parcels
-  County Centerline Names
-  County Centerlines
-  Blueline Streams
-  City Areas
-  World Street Map

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0 385 770 Feet



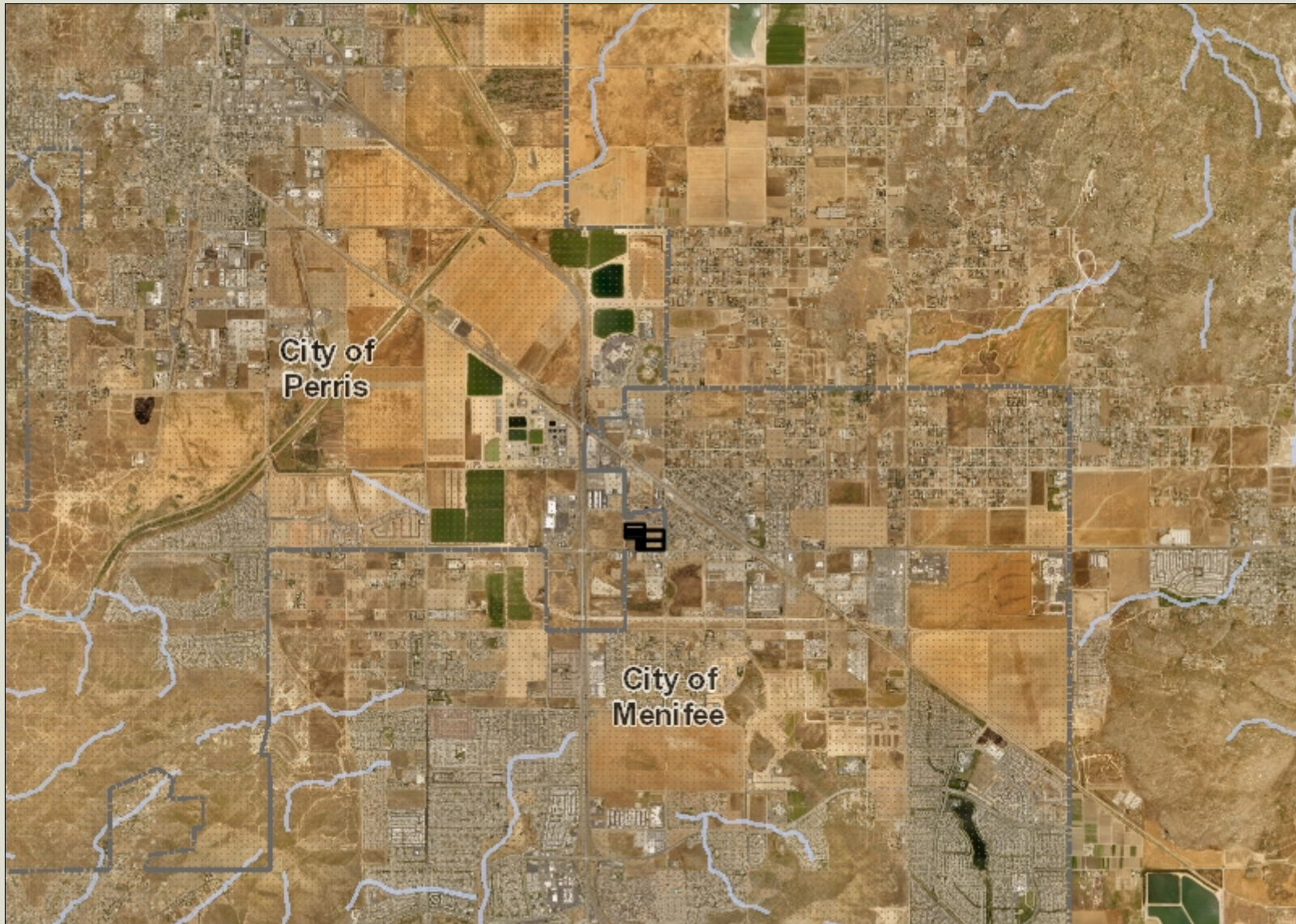
A horizontal scale bar with markings at 0, 385, and 770 feet.

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Notes

Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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Notes



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RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



September 14, 2023

Jennifer Lopez, Project Planner
County of Riverside Planning Department
4080 Lemon Street, 12th Floor
Riverside CA 92501

CHAIR
Steve Manos
Lake Elsinore

VICE CHAIR
Russell Betts
Desert Hot Springs

COMMISSIONERS

John Lyon
Riverside

Richard Stewart
Moreno Valley

Steven Stewart
Palm Springs

Michael Geller
Riverside

Vernon Poole
Murrieta

STAFF

Director
Paul Rull

Simon Housman
Jackie Vega
Barbara Santos

County Administrative Center
4080 Lemon St., 14th Floor.
Riverside, CA 92501
(951) 955-5132

www.rcaluc.org

RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW – DIRECTOR’S DETERMINATION

File No.: ZAP1583MA23
Related File No.: TPM 38358 (Tentative Parcel Map)
APN: 343-090-004
Airport Zone: Zone E

Dear Ms. Lopez:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) as authorized pursuant to Section 1.5.2(d) of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed County of Riverside Case No. TPM 38358 (Tentative Parcel Map), a proposal to divide 10.20 acres into two residential lots, located southerly of Elis Avenue, northerly of Floral Avenue, easterly of Belita Drive, and westerly of Palms Street.

The project is located within Compatibility Zone E of March Air Reserve Base/Inland Port Airport Influence Area, where Zone E does not restrict residential density.

Although the project is located within the March Air Reserve Base/Inland Port Airport Influence Area, the nearest runway is actually Runway 15-33 at Perris Valley Airport. The elevation of Runway 15-33 at Perris Valley Airport is approximately 1,417 feet above mean sea level (AMSL) at its northerly terminus. At a distance of 20,328 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with an elevation at top of roof exceeding 1,620 feet AMSL. The site elevation is 1,454 feet AMSL, and the proposed building height is 22 feet, resulting in a top point elevation of 1,476 feet AMSL. Therefore, FAA OES review for height/elevation was not required.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT**, with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, provided the County of Riverside applies the following recommended conditions:

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or

amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.

- (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, outdoor production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Highly noise-sensitive outdoor nonresidential uses.
 - (f) Any use which results in a hazard to flight, including physical (e.g., tall objects), visual, and electronic forms of interference with the safety of aircraft operations.
3. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property.
4. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the stormwater basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the stormwater basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the stormwater basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin

If you have any questions, please contact me at (951) 955-6893.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A handwritten signature in blue ink that reads "Paul Rull". The signature is written in a cursive, flowing style.

Paul Rull, ALUC Director

Attachments: Notice of Airport in Vicinity

cc: Eduardo Cobian (applicant/representative/property owner)
Gary Gosliga, March Inland Port Airport Authority
Major. David Shaw, Base Civil Engineer, March Air Reserve Base
ALUC Case File

X:\AIRPORT CASE FILES\March\ZAP1583MA23\ZAP1583MA23 LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

**THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS**

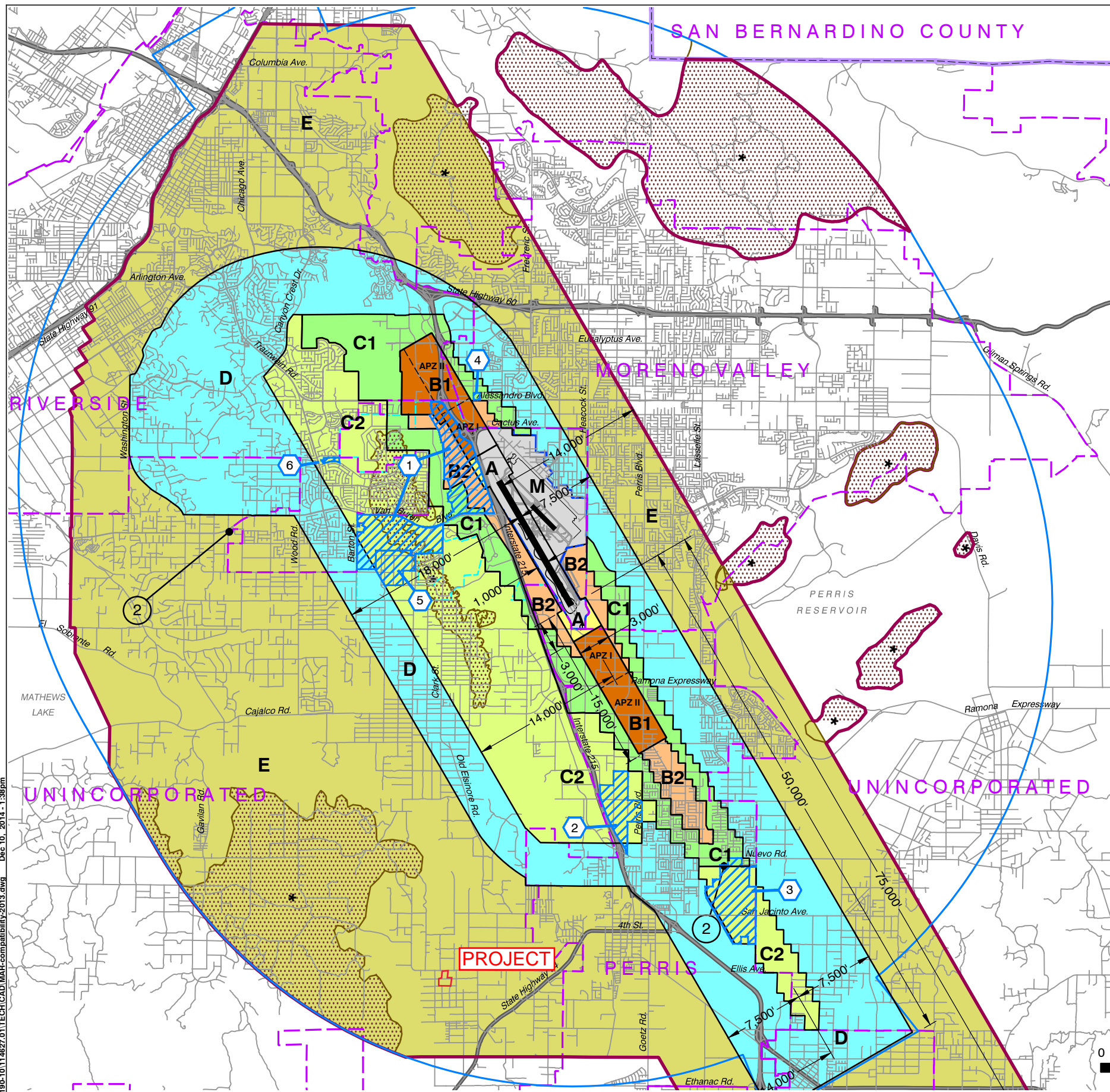
**PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES**



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



LEGEND

Compatibility Zones

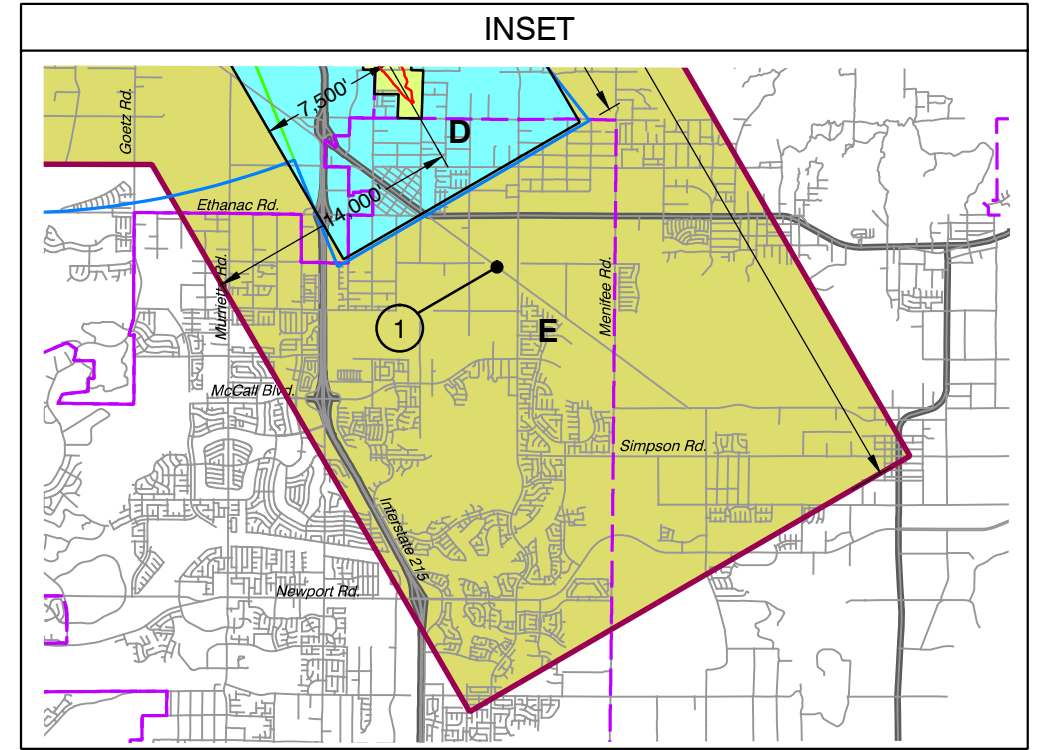
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

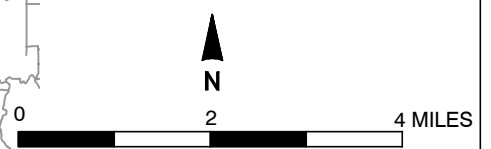
- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



**Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)**

Note:
All dimensions are measured from runway ends and centerlines.



Base map source: County of Riverside 2013

X:\18190-10\114627\01\TECH\CAD\MAR-compatibility\2013.dwg Dec 10, 2014 - 1:38pm

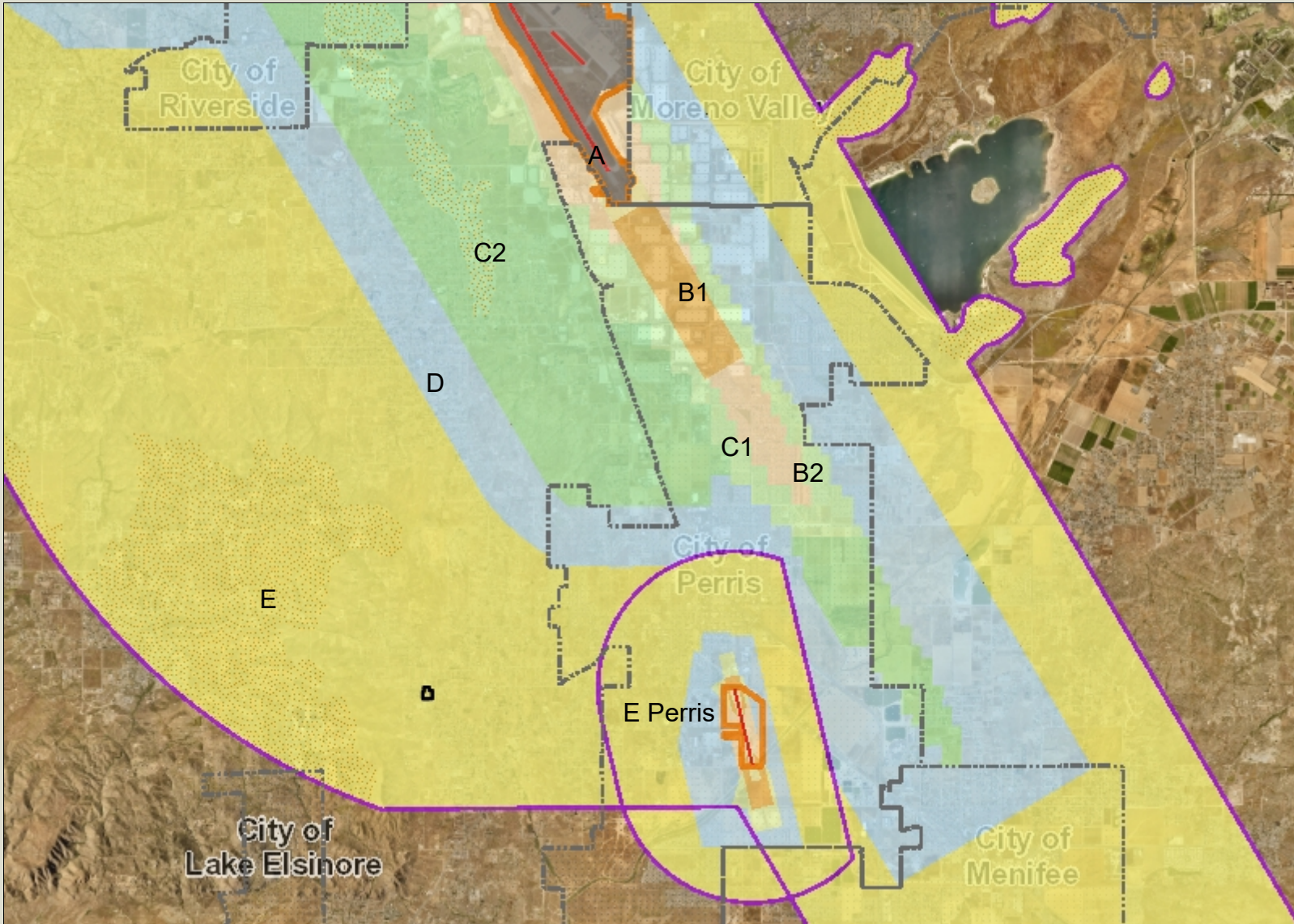
Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map MA-1

**Compatibility Map
March Air Reserve Base / Inland Port Airport**

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas

Airport Compatibility Zones

- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

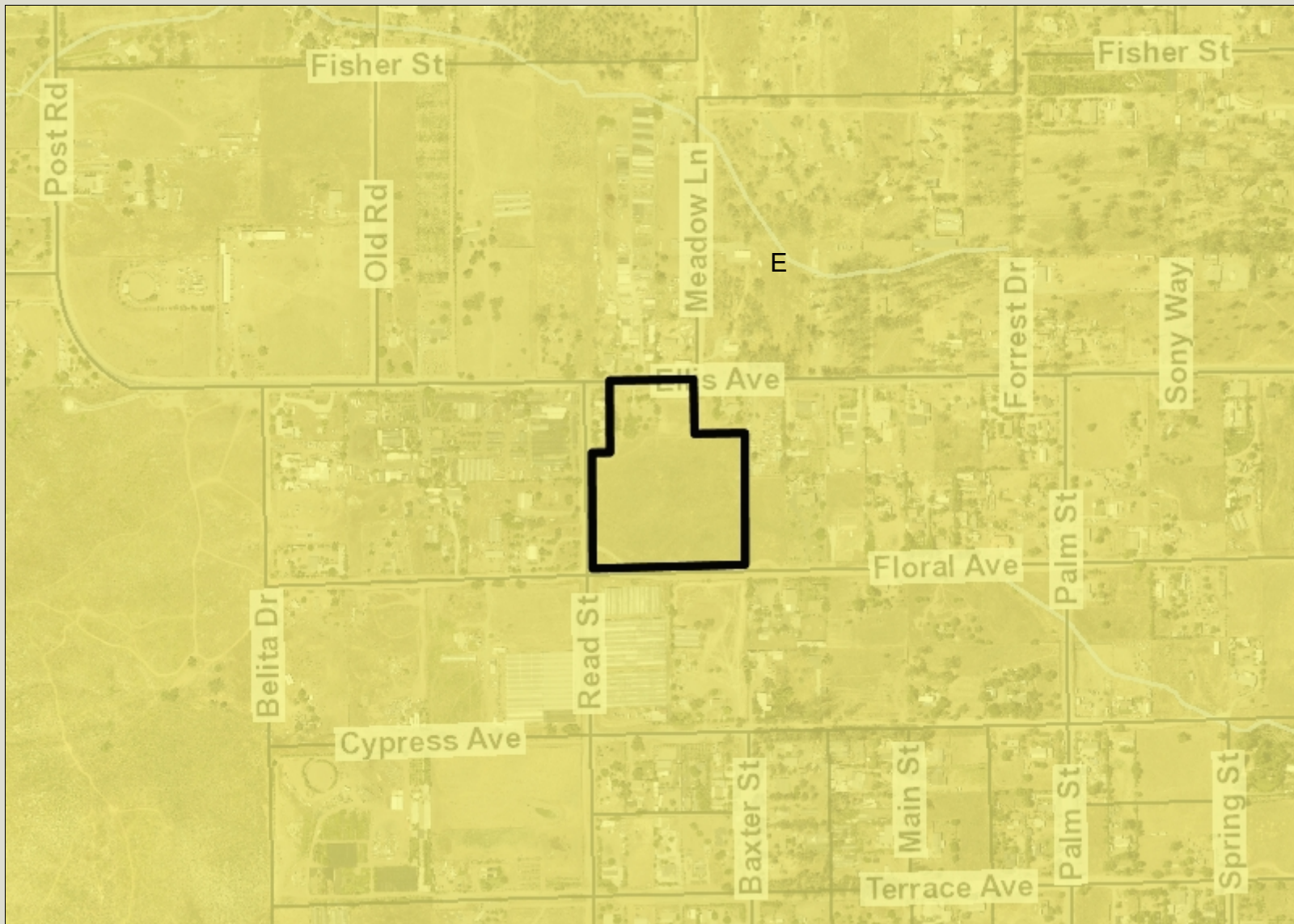


REPORT PRINTED ON... 9/14/2023 11:21:57 AM

© Riverside County GIS

Notes

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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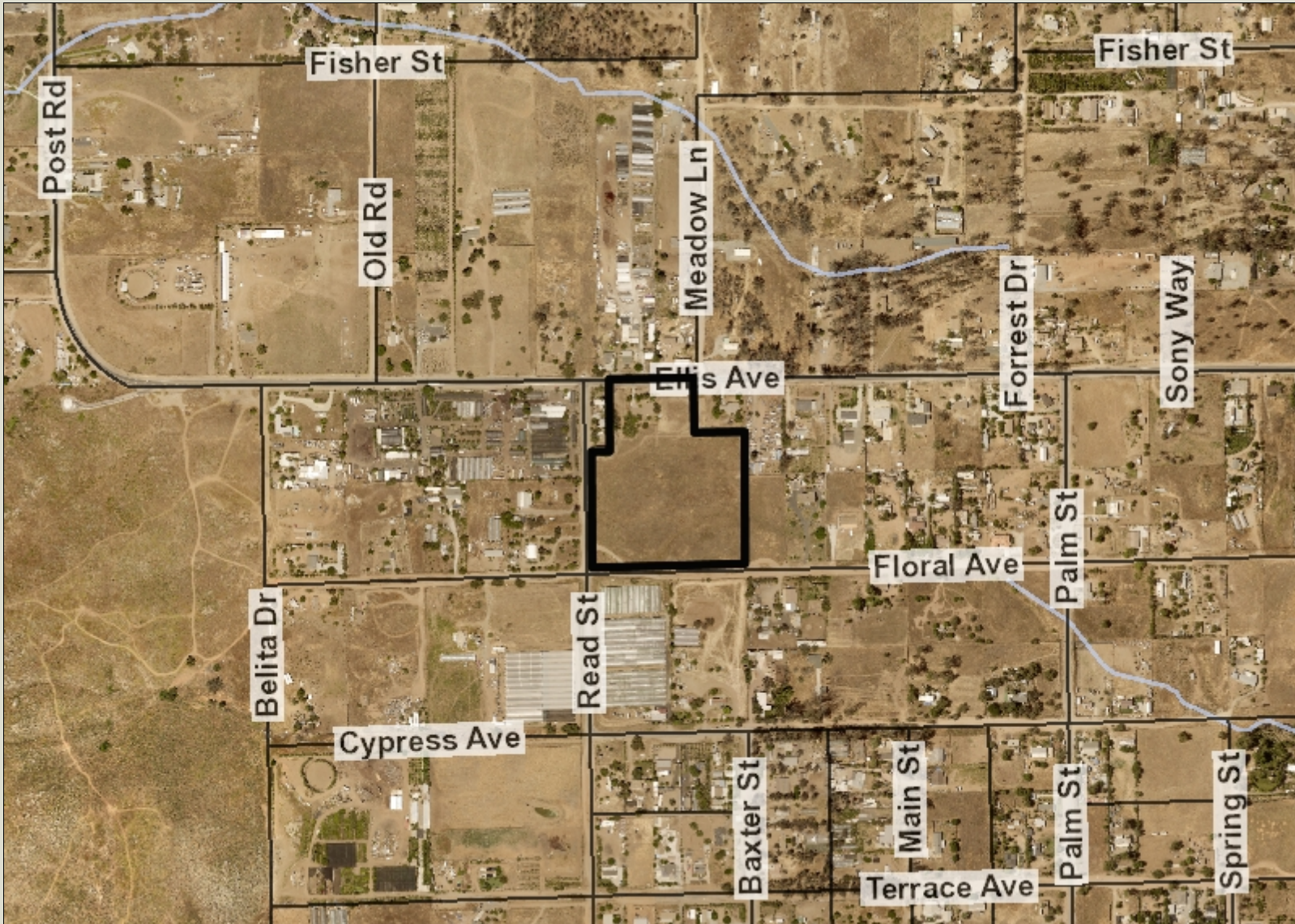
0 770 1,539 Feet

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Notes

Map My County Map



Legend

- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



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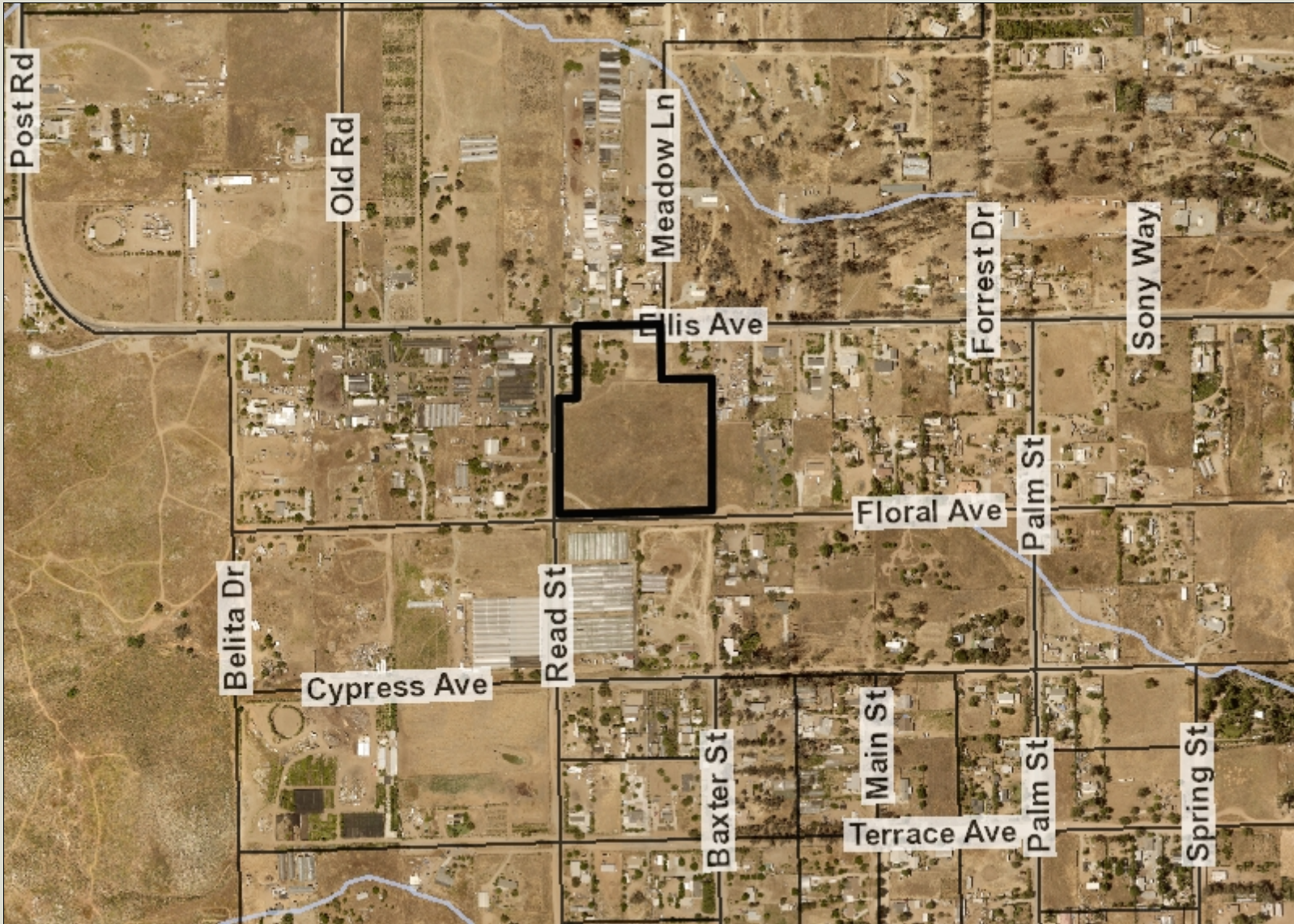
Notes

0 770 1,539 Feet

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Map My County Map



Legend

- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



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Notes

0 770 1,539 Feet

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Map My County Map



Legend

-  Parcels
-  County Centerline Names
-  County Centerlines
-  Blueline Streams
-  City Areas
-  World Street Map



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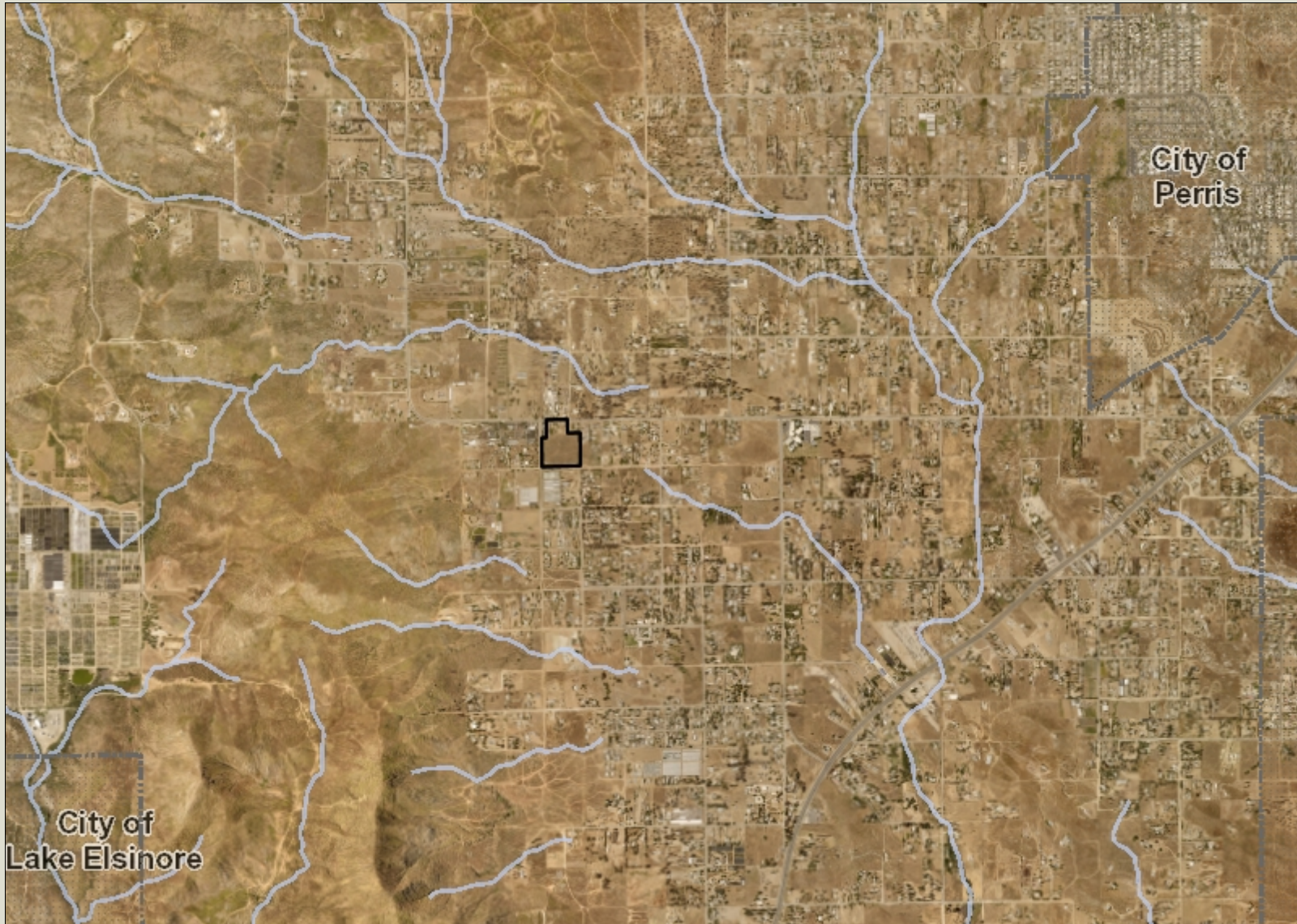
0 385 770 Feet

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Notes

Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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Notes

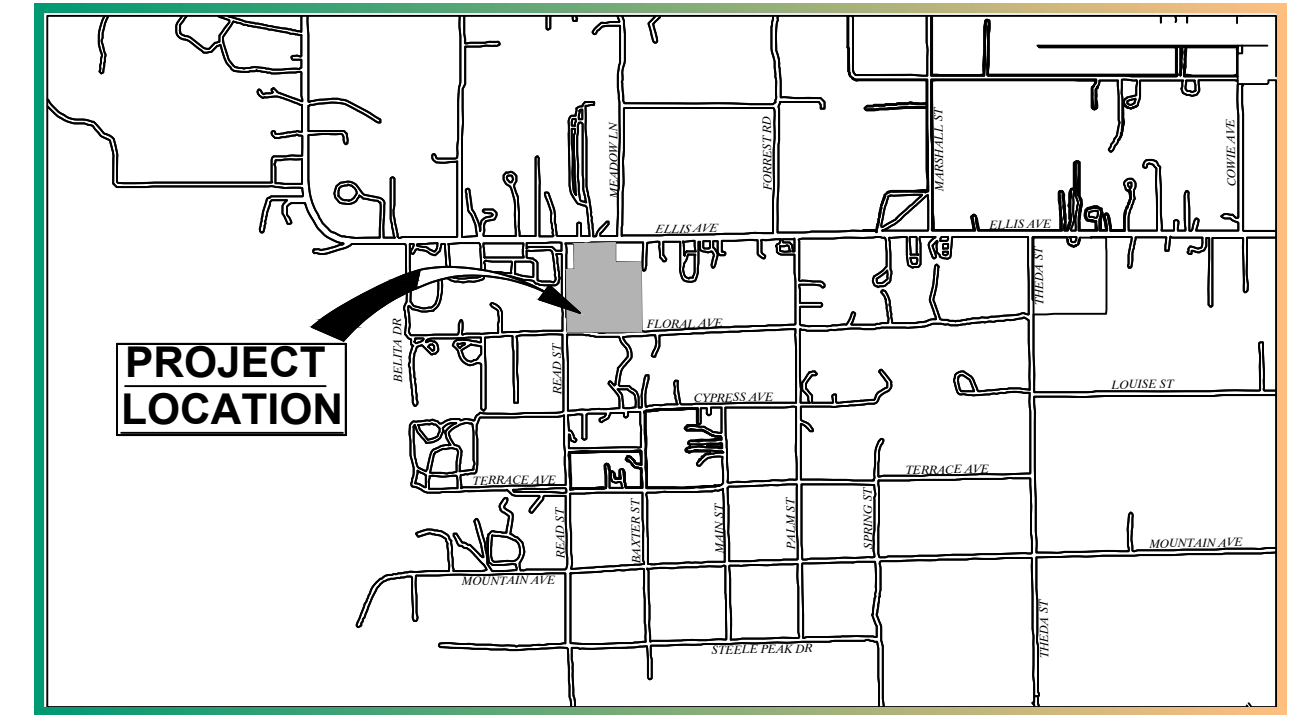


REPORT PRINTED ON... 9/14/2023 11:25:57 AM

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TENTATIVE PARCEL MAP 38358

COUNTY OF RIVERSIDE



VICINITY MAP

LOT OR PARCEL	GROSS AREA		NET AREA		EXISTING ZONNING	PROPOSED ZONNING	USE
	AC	SQ FT	AC	SQ FT			
PARCEL 1	5.00	217,807.15	4.45	193,921.14	R-R	R R	RURAL
PARCEL 2	5.28	229,996.80	4.64	201,948.73	R-R	R R	RURAL
TOTAL	10.28	447,803.95	9.09	395,869.87			

OFFICIAL USE ONLY:

**LAND OWNER(S)
DEVELOPER(S)**

COBIAN, CARLOS E; COBIAN, ELVIA
21289 ELLIS AVE, PERRIS, CA 92570-8208

SITE ADDRESS:

21289 ELLIS AVE PERRIS,
RIVERSIDE COUNTY,
CA 92570-8208
APN: 343-090-004

LEGAL DESCRIPTION

9.41 ACRES M/L IN A PORTION OF LOT 6
MOUNTAIN GLEN TRACT, IN THE COUNTY OF
RIVERSIDE, STATE OF CALIFORNIA, AS SHOWN
BY MAP ON FILE IN BOOK 11 PAGE 520 OF
SAN DIEGO COUNTY RECORDS.

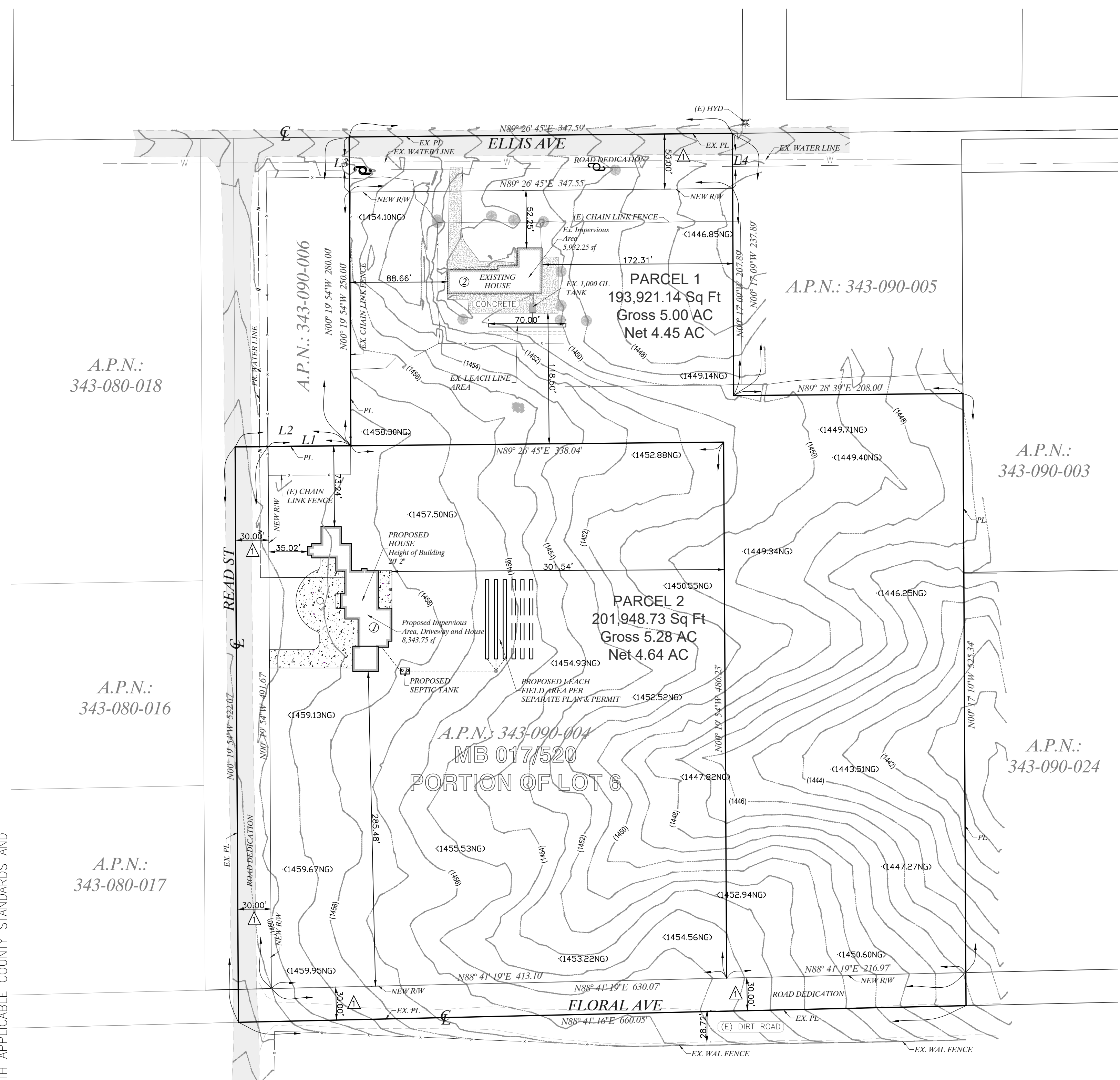
BENCHMARK: DX2129

NAME BENCHMARK: DX2129
ELEV: 1642.59 Feet
VERTICAL DATUM: NAVD 88

RECOVERY NOTE BY METRO WATER DISTR SO.
CALIFORNIA 1992 PERRIS, 2.65 MILES (4.26
KM) WEST ALONG CAJALCO RD FROM THE
215 FRWY, 600 FEET (182.9 M) EAST OF
THE INTERSECTION OF CAJALCO RD AND
BROWN ST, 45 FEET (13.7 M) SOUTH OF
THE CENTERLINE OF CAJALCO RD. A
STANDARD MWDSC 3-1/4 INCH BRASS DISK
SET IN THE NORTHSIDE OF GRANITE BOULDER
ON THE NORTH SIDE OF A SMALL GROUP OF
ROCKS.

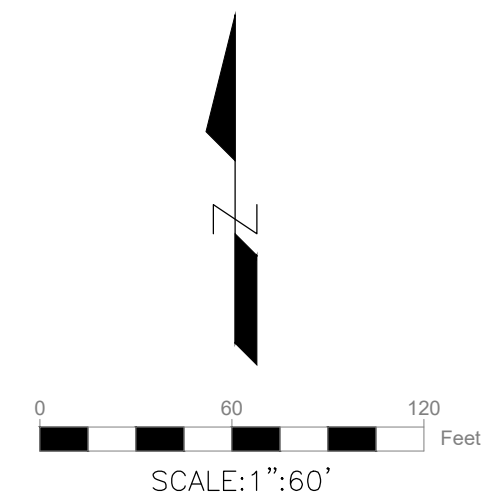
SOURCE OF TOPOGRAPHY

TOPOGRAPHIC SURVEY WAS PERFORMED BY
SERVITOP ENGINEERING ON JULY 2021.
USING \odot OF VERBENA DR. THE BASIS OF
BEARING IS N0°15'30"W PER
MB 024-020.



LINE AND BEARING DATA:

L1	N89°26'45"E	75.00'
L2	N89°26'45"E	105.00'
L3	N00°19'54"E	50.00'
L4	N00°17'09"E	50.00'



CONSTRUCTION NOTES:

- ① EXISTING HOUSE
- ② PROPOSED HOUSE

GENERAL NOTES:

1-THIS TENTATIVE MAP INCLUDES THE
ENTIRE CONTIGUOUS OWNERSHIP OF THE
LAND DIVIDER

UTILITY PURVEYORS:

WATER:	CITY OF PERRIS FINANCE & WATER	(909)943-4610
TELEPHONE:	AT&T	(800)310-2355
GAS:	THE GAS COMPANY	(800)427-2200
ELECTRICITY:	SOUTHERN CALIFORNIA EDISON	(800)684-8123
CABLE/TV:	DIRECTV	(877)916-5137
INTERNET:	SPECTRUM	(855)243-8892

ROAD DEDICATION:

- ▲ PROPOSED OFFER ROAD DEDICATION
FOR PURPOSES STREET

LEGEND

	PROPERTY LINE
	CENTER LINE
	EXISTING CHAINLINK
	EXIST. CHAINLINK FENCE
	EXIST. TREE
	PCC CONCRETE
	EXISTING POWER POLE
	EXISTING ASPHALT

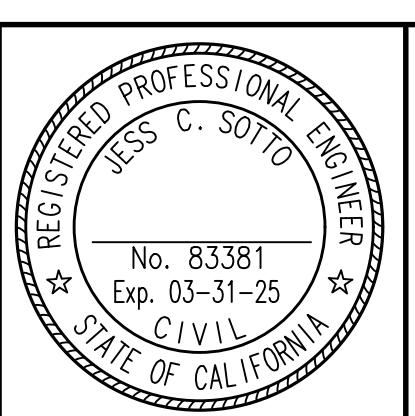
PLAN CHECK OVERSIGHT ENGINEER
REGISTRATION NUMBER
DATE SIGNED

APPROVED AS TO CONFORMANCE WITH APPLICABLE COUNTY STANDARDS AND PRACTICES.

NOTE:
WORK CONTAINED WITHIN THESE PLANS SHALL NOT COMMENCE UNTIL AN ENCROACHMENT PERMIT AND/OR A GRADING PERMIT HAS BEEN ISSUED.

THE PRIVATE ENGINEER SIGNING THESE PLANS IS RESPONSIBLE FOR ASSURING THE ACCURACY AND ACCEPTABILITY OF THE DESIGN HEREON. IN THE EVENT OF DISCREPANCIES ARISING AFTER COUNTY APPROVAL OR DURING CONSTRUCTION, THE PRIVATE ENGINEER SHALL BE RESPONSIBLE FOR DETERMINING AN ACCEPTABLE SOLUTION AND REVISING THE PLANS FOR APPROVAL BY THE COUNTY.

MARK	BY	DATE	REVISIONS	APPR.	DATE



DATE: 06/19/2023
JESS C. SOTTO R.C.E. No. 83381 EXP.03/31/25

SCALE: H: _____ V: _____

TENTATIVE PARCEL MAP
A.P.N.: 343-090-004

FOR: _____ W.O. _____ COUNTY FILE NO. _____

SHEET NO. **1** OF 1 SHTS



**AIRPORT LAND USE COMMISSION MEETING
MINUTES
September 14, 2023**

DRAFT

9-15-23

COMMISSIONERS PRESENT: Michael Geller, John Lyon, Steve Manos, Richard Stewart, Jeffrey Hewitt (alternate for Vernon Poole), Michael Lewis, (alternate for Steven Stewart)

COMMISSIONERS ABSENT: Vernon Poole, Steven Stewart, Russell Betts

2.0 PUBLIC HEARING: CONTINUED ITEMS

None

3.0 PUBLIC HEARING: NEW CASES

3.1 Staff report recommended: **CONSISTENT**
Staff recommended at hearing: **CONSISTENT**
ALUC Commission Action: **CONSISTENT (Vote 5-0; Recuse: Jeffrey Hewitt)**
Motion: Michael Lewis
Second: Richard Stewart

ZAP1111RI23 – 11th & Olive Street, LLC (Representative: Overland Development Company) City of Riverside Case No. PR-2023-001474 (General Plan Amendment, Change of Zone, Development Review). A proposal to construct 255 multi-family residential units on 3.9 acres, located on the southeast corner of Van Buren Boulevard and Duncan Avenue. The applicant also proposes to amend General Land Use designation from High Density Residential (HDR) and Commercial (C) to Very High Density Residential (VHDR), and to rezone the property from R-3-1,500 (Multi-Family Residential) and R-1-7000 (Single Family Residential) to R-4 Multi-Family Residential. (Airport Compatibility Zone E of the Riverside Municipal Airport Influence Area). Staff Planner: Jackie Vega at (951) 955-0982, or e-mail at javega@rivco.org

3.2 Staff report recommended: **INCONSISTENT**
Staff recommended at hearing: **INCONSISTENT**
ALUC Commission Action: **INCONSISTENT (Vote 5-0; Recuse: Jeffrey Hewitt)**
Motion: Michael Geller
Second: Michael Lewis

ZAP1548MA22 – Anton Mission Grove LLC (Representative: Overland Devco) City of Riverside Case No. PR-2022-001359 (General Plan Amendment, Specific Plan Amendment, Rezone, Development Plan Review). A proposal to construct a multi-family development consisting of 347 multi-family residential units, pool area, leasing office, club area, and fitness center on 9.92 acres, located on the northwest corner of Mission Grove Parkway and Mission Village Drive. The applicant also proposes amending the site's General Plan land use designation from C-Commercial to MU-U-Mixed Use Urban, and rezoning the site from the site from CR-Commercial Retail to MU-U-Mixed Use-Urban, and a specific plan amendment to amend the Mission Grove Specific Plan to permit mixed use/multi-family residential units on the project site. The applicant also proposes 40,000 square feet of solar panel area on the building's rooftops and carports (Airport Compatibility Zone C2 of the March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

VIDEO:

1

A video recording of the entire proceedings is available on the ALUC website at www.rcaluc.org. If you have any questions please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at basantos@rivco.org

**AIRPORT LAND USE COMMISSION MEETING
MINUTES
September 14, 2023**

4.0 PUBLIC HEARING: MISCELLANEOUS ITEMS

None

5.0 ADMINISTRATIVE ITEMS

5.1 Director's Approvals - Information Only

5.2 Update March Air Reserve Base Compatibility Use Study (CUS)

Simon Housman, Project Manager for the March CUS informed the Commission that the last piece of the study is the Cumulative Impact Solar Glare. Most recently with the cooperation of the March ARB public affairs, they are in the process of getting photographs and videos of actual glare onsite to incorporate into the study as the final piece to the MCUS.

6.0 APPROVAL OF MINUTES

Commissioner Michael Geller motioned to approve the August 10, 2023 minutes. Seconded by Commissioner Richard Stewart. (Vote 5-0; Abstain: Commissioner Hewitt)

7.0 ORAL COMMUNICATION ON ANY MATTER NOT ON THE AGENDA

Jackie Vega, Staff Planner informed the Commission regarding updates to the ALUC website.

8.0 COMMISSIONER'S COMMENTS

None

9.0 ADJOURNMENT

Steve Manos, Chair adjourned the meeting at 10:14 a.m.

Y:\ALUC COMMISSION - PUBLIC HEARING\ALUC Minutes\2023 Minutes\Minutes 9-14-23.doc

VIDEO:

2

A video recording of the entire proceedings is available on the ALUC website at www.rcaluc.org. If you have any questions please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at basantos@rivco.org