



# AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY AGENDA

Riverside County Administration Center  
4080 Lemon St., 1st Floor Hearing Room  
Riverside, California

Thursday 9:00 a.m., April 10, 2014

**CHAIR**  
Simon Housman  
Rancho Mirage

**VICE CHAIRMAN**  
Rod Ballance  
Riverside

**COMMISSIONERS**

Arthur Butler  
Riverside

Glen Holmes  
Hemet

John Lyon  
Riverside

Greg Pettis  
Cathedral City

Richard Stewart  
Moreno Valley

**NOTE:** If you wish to speak, please complete a "SPEAKER IDENTIFICATION FORM" and give it to the Secretary. The purpose of the public hearing is to allow interested parties to express their concerns. Comments shall be limited to 5 minutes and to matters relevant to the item under consideration. Please do not repeat information already given. If you have no additional information, but wish to be on record, simply give your name and address and state that you agree with the previous speaker(s). Also please be aware that the indicated staff recommendation shown below may differ from that presented to the Commission during the public hearing.

Non-exempt materials related to an item on this agenda submitted to the Airport Land Use Commission or its staff after distribution of the agenda packet are available for public inspection in the Airport Land Use Commission's office located at 4080 Lemon Street, 14<sup>th</sup> Floor, Riverside, CA 92501 during normal business hours.

In compliance with the Americans with Disabilities Act, if any accommodations are needed, please contact Barbara Santos at (951) 955-5132 or E-mail at [basantos@rctlma.org](mailto:basantos@rctlma.org). Request should be made at least 48 hours or as soon as possible prior to the scheduled meeting.

## 1.0 INTRODUCTIONS

1.1 CALL TO ORDER

1.2 SALUTE TO FLAG

1.3 ROLL CALL

## 2.0 PUBLIC HEARING: NEW CASE

### PALM SPRINGS INTERNATIONAL AIRPORT

2.1 ZAP1020PS14 – Spectrum Services, Inc. (Representative: Brett Smirl/Michael Hayes)  
City of Palm Springs Case Nos. CUP 5.1314 (Conditional Use Permit) and VAR 6.530 (Variance). CUP 5.1314 is a proposal to establish an unmanned telecommunications facility consisting of antennas on a 48-foot high monopalm tower, with associated equipment shelter, on a 900 square foot lease area within a 0.39-acre parcel located at the southeasterly corner of Sahara Road and (North) Cerritos Road in the City of Palm Springs. VAR 6.530 is a proposal to allow the 48-foot high structure in the P (Professional) Zone. Without a variance, the allowable height limit for antennas is 15 feet. (Zone B1 of the Palm Springs International Airport Influence Area).

Staff Recommendation: CONSISTENT

**CHAIR**

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Rancho Mirage

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**STAFF**

Director  
Ed Cooper

John Guerin  
Russell Brady  
Barbara Santos

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Riverside, CA 92501  
(951) 955-5132

[www.rcaluc.org](http://www.rcaluc.org)

**3.0 ADMINISTRATIVE ITEMS**

3.1 Director's Approvals

3.2 Compatibility Plan Status Update

3.3 2014 California Airport Land Use Consortium Conference

**4.0 APPROVAL OF MINUTES**

March 13, 2014

**5.0 ORAL COMMUNICATION ON ANY MATTER NOT ON THE AGENDA**

**6.0 COMMISSIONER'S COMMENTS**

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**COUNTY OF RIVERSIDE  
AIRPORT LAND USE COMMISSION**

**STAFF REPORT**

**AGENDA ITEM:** 2.1

**HEARING DATE:** April 10, 2014

**CASE NUMBER:** ZAP1020PS14 – Spectrum Services (Representatives: Michael Hayes and Brett Smirl)

**APPROVING JURISDICTION:** City of Palm Springs

**JURISDICTION CASE NO:** CUP 5.1314 (Conditional Use Permit); VAR 6.530 (Variance)

**MAJOR ISSUES:** The Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, as carried forth into the 2005 Palm Springs International Airport Land Use Compatibility Plan, cite “critical community infrastructure facilities” as a prohibited use in Airport Compatibility Zone B1. These facilities are listed in Note 12 of Table 2A as including “public communications facilities.” Policy 4.2.3.(d) clarifies that such uses are “prohibited unless no other feasible alternative site exists and the facility is designed in a manner that minimizes its susceptibility to damage from an aircraft accident.”

City staff has indicated that, although the City does not have specific standards restricting cell towers on or near residential zoned properties, previous proposals to install cell towers near residential land uses have been denied. Two other monopalm cell towers currently exist on the site, and the proposed tower would be clustered with these existing towers within approximately 40 feet. The proposed tower is 48 feet in height, whereas the existing towers are approximately 53 feet and 57 feet in height. Therefore, due to the clustering and existing towers’ heights, the proposed tower would not “have a substantial adverse effect on the safe and efficient utilization of the navigable airspace by aircraft,” as determined by the Federal Aviation Administration. The applicant and City staff have also indicated a preference to locate the proposed tower near the existing towers in order to avoid creating new visual impacts in other areas as a result of siting the tower either outside Zone B1 or further from the extended runway centerline within Zone B1.

Staff has received propagation maps showing the existing and proposed coverage to indicate the area where coverage is needed and thus where a tower should be located. ALUC staff’s review of aerial photos of the area within the radius provided by the applicant indicate that there are a few vacant or underdeveloped areas both within Compatibility Zone C and further from the extended runway centerline within Compatibility Zone B1 that may be feasible alternative sites. However, selection of an alternative site (whether within or outside Compatibility Zone B1) could potentially create a new hazard where one does not currently

exist (in contrast to use of the current proposed site where obstruction impacts would not be significantly increased).

**RECOMMENDATION:** Staff recommends that the Commission open the public hearing, consider testimony, and find the project CONSISTENT, subject to the conditions included herein.

**PROJECT DESCRIPTION:**

City of Palm Springs Case CUP 5.1314 is a proposal to establish an unmanned telecommunications facility consisting of antennas on a 48-foot high monopalm tower, with associated equipment shelter, on a 900 square foot lease area within a 0.39-acre parcel. VAR 6.530 is a proposal to allow the 48-foot high structure in the P (Professional) Zone. Without a variance, the allowable height limit for antennas is 15 feet.

**PROJECT LOCATION:**

The site is located northerly of Vista Chino, easterly of North Cerritos Road, and southerly of Sahara Road in the City of Palm Springs, approximately 2,400 feet northwesterly of Runway 13R-31L at Palm Springs International Airport.

**LAND USE PLAN:** 2005 Palm Springs International Airport Land Use Compatibility Plan

- a. Airport Influence Area: Palm Springs International Airport
- b. Land Use Policy: Compatibility Zone B1
- c. Noise Levels: between 60-65 CNEL from aircraft noise

**BACKGROUND:**

Prohibited Uses: "Critical community infrastructure facilities" are cited as a prohibited use in Airport Compatibility Zone B1 pursuant to the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan (as carried forth into the 2005 Palm Springs International Airport Land Use Compatibility Plan). These facilities are listed in Note 12 of Table 2A as including "public communications facilities." Policy 4.2.3.(d) clarifies that such uses are "prohibited unless no feasible alternative site exists and the facility is designed in a manner that minimizes its susceptibility to damage from an aircraft accident."

The prohibition likely relates to the role that these facilities play in the maintenance of public safety in an emergency situation. There is no general prohibition of new structures in Airport Compatibility Zone B1. Obviously, as an unmanned facility, the project does not present intensity issues.



City staff has indicated that, although the City does not have specific standards restricting cell towers on or near residential zoned properties, previous proposals to install cell towers near residential land uses have been denied. Two other monopalm cell towers currently exist on the site, and the proposed tower would be clustered with these existing towers within approximately 40 feet. The proposed tower is 48 feet in height, whereas the existing towers are approximately 53 feet and 57 feet in height. Therefore, due to the clustering and existing towers' heights, the proposed tower would not "have a substantial adverse effect on the safe and efficient utilization of the navigable airspace by aircraft," as determined by the Federal Aviation Administration. The applicant and City staff have also indicated a preference to locate the proposed tower near the existing towers in order to avoid creating new visual impacts in other areas as a result of siting the tower either outside Zone B1 or further from the extended runway centerline within Zone B1.

Staff has received propagation maps showing the existing and proposed coverage to indicate the area where coverage is needed and thus where a tower should be located. ALUC staff's review of aerial photos of the area within the radius provided by the applicant indicate that there are a few vacant or underdeveloped areas both within Compatibility Zone C and further from the extended runway centerline within Compatibility Zone B1 that may be feasible alternative sites.

The applicant has indicated that inquiries had been made as to availability of locations on the commercial sites (located northwest of Sunrise Way and Vista Chino and southwest of Cerritos Drive and Vista Chino), but the owners of these properties were not interested in accommodating a cell tower due to planned development of their properties. Staff has identified a third potential location - a vacant property located west of Sunrise Way, north of Sandalwood Drive. This site, although located within the applicant's search radius, would not meet the applicant's objective to provide coverage to the more northeasterly area, where the current substantial coverage gap occurs. Although other vacant or underdeveloped sites may exist within the area, many of these lie within primarily residential areas, thus limiting the feasibility of these sites. In addition, selection of an alternative site (whether within or outside Compatibility Zone B1), could potentially create a new hazard where one does not currently exist (in contrast to the current proposed site where obstruction impacts would not be significantly increased).

Extended Runway Centerline: Pursuant to Table 2A, structures in Airport Compatibility Zone B1 should be located a "maximum distance from [the] extended runway centerline." This project does not comply with this requirement, as the facility is not located at a maximum distance from the centerline on the subject parcel. The proposed tower would be clustered with the other existing monopalms on site per the City of Palm Springs to limit aesthetic impacts and allow the proposed monopalm to blend in with its surroundings. Unfortunately, this portion of the site is close to the extended runway centerline. Within this parcel, the tower may only be shifted approximately 80 feet further from the extended runway centerline compared to its current proposed location.

Part 77: The property is located approximately 2,400 feet northwesterly of Runway 13R-31L. Based on this distance and the approximate runway elevation of 474.4 feet above mean sea level (AMSL), any structure exceeding 498.4 feet AMSL would require FAA Obstruction Evaluation review. The site has an elevation of approximately 504 feet AMSL, and the project proposes a structure height of

48 feet for a total elevation of 552 feet AMSL. Submittal to FAA was made for Obstruction Evaluation and was assigned an Aeronautical Study Number (ASN) of 2013-AWP-7273-OE.

The aeronautical study determined that the structure exceeds Section 77.17(a)(3) obstruction standards in that it would penetrate the PSP RWY 31L 40:1 departure obstacle clearance surface (OCS) in the Initial Climb Area (ICA) by 14 feet. However, the proposed structure height would not require an increase in the existing published instrument departure climb gradient, nor would it require an increase in departure weather minimums.

The study noted the presence of structures of similar height adjacent to the proposed site. The Obstruction Evaluation Service did not attempt to negotiate a lower height, since existing obstacles and terrain control the development of future approach and departure Terminal Instrument Procedures for the PSP landing area.

As the structure would qualify as a "low close-in" obstacle penetration, upon its development, a note will be added to the Take-off Minimums and (Obstacle) Departure Procedures in the U.S. Terminal Procedures publication. Additionally, it was determined that the structure shall be red obstruction lighted to provide conspicuity for aircraft operators flying in VFR weather conditions at night.

A Determination of No Hazard to Air Navigation was issued, based on a finding "that the structure would have no substantial adverse effect on the safe and efficient utilization of the navigable airspace by aircraft or on the operation of air navigation facilities."

Noise: Average noise levels on this site from aircraft operations would be between 60 and 65 dB CNEL. As a non-noise sensitive use, no special mitigation measures are necessary.

**CONDITIONS:**

1. Prior to issuance of a building permit for the proposed telecommunications facilities, the property owner shall convey an aviation easement to the City of Palm Springs as owner-operator of Palm Springs International Airport.
2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.

- (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area, including landscaping utilizing water features, aquaculture, livestock operations, production of cereal grains, sunflower, and row crops, artificial marshes, landfills, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, incinerators, fly ash disposal, and wastewater management facilities.
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Children's schools, day care centers, libraries, hospitals, nursing homes, places of worship, buildings with greater than 2 aboveground habitable floors, highly noise-sensitive outdoor nonresidential uses, aboveground bulk storage of hazardous materials, and hazards to flight.
3. Any outdoor lighting that is installed other than FAA-required lighting shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky.
  4. Any new retention basins on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the retention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. In the event that a retention basin or detention basin is established on this site, on-site landscaping shall not include trees that produce seeds, fruits, or berries.
  5. The Federal Aviation Administration has conducted an aeronautical study of the proposed structure (Aeronautical Study No. 2013-AWP-7273-OE), and has determined that lighting of the structure in accordance with FAA Advisory Circular 70/7460-1 K Change 2, Obstruction Marking and Lighting, red lights-Chapters 4, 5 (Red), and 12, will be necessary for aviation safety. Such lighting shall be installed and maintained in accordance therewith for the life of the project.
  6. The maximum elevation at the top of the proposed structure shall not exceed 552 feet above mean sea level.
  7. The specific coordinates, height, top point elevation, of the proposed structure, frequencies, and power specified in the Federal Aviation Administration letter dated February 13, 2014, shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however that reduction in structure height or elevation shall not require further review by the Airport Land Use Commission.

8. Temporary construction equipment (such as cranes) used during actual construction of the structure shall not exceed the height of the structure or be stationed at coordinates that are closer to the runway than the coordinates specified in the Federal Aviation Administration letter dated February 13, 2014, unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.
9. Within five (5) days after construction of structures reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned.
10. The telecommunications facility shall be designed in such a manner as to ensure that spurious emissions signal levels from the proposed transmitter(s) will be less than -104 dBm in the 108-137 and 225-400 MHz frequency bands at a distance of 4,100 feet from the transmitter site, in accordance with the requirements of the Federal Aviation Administration Obstruction Evaluation Service letter dated February 13, 2014, a copy of which is attached hereto and incorporated herein by reference.
11. The proposed monopalm tower shall be located within forty feet (40') of the existing monopalm towers on the site to accommodate the clustering of the towers to minimize obstruction hazard.

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)





Mail Processing Center  
 Federal Aviation Administration  
 Southwest Regional Office  
 Obstruction Evaluation Group  
 2601 Meacham Boulevard  
 Fort Worth, TX 76193

Aeronautical Study No.  
 2013-AWP-7273-OE

Issued Date: 02/13/2014

Jim O'Dowd  
 Verizon Wireless  
 180 Washington Valley Rd  
 Bedminster, NJ 07921

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Monopole Desert Park
Location:	Palm Springs, CA
Latitude:	33-50-43.81N NAD 83
Longitude:	116-31-24.26W
Heights:	504 feet site elevation (SE)
	48 feet above ground level (AGL)
	552 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure would have no substantial adverse effect on the safe and efficient utilization of the navigable airspace by aircraft or on the operation of air navigation facilities. Therefore, pursuant to the authority delegated to me, it is hereby determined that the structure would not be a hazard to air navigation provided the following condition(s) is(are) met:

As a condition to this Determination, the structure is marked/lighted in accordance with FAA Advisory circular 70/7460-1 K Change 2, Obstruction Marking and Lighting, red lights - Chapters 4,5(Red),&12.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

Any height exceeding 48 feet above ground level (552 feet above mean sea level), will result in a substantial adverse effect and would warrant a Determination of Hazard to Air Navigation.

This determination expires on 08/13/2015 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.

- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is subject to review if an interested party files a petition that is received by the FAA on or before March 15, 2014. In the event a petition for review is filed, it must contain a full statement of the basis upon which it is made and be submitted to the Manager, Airspace Regulations & ATC Procedures Group, Federal Aviation Administration, 800 Independence Ave, SW, Room 423, Washington, DC 20591.

This determination becomes final on March 25, 2014 unless a petition is timely filed. In which case, this determination will not become final pending disposition of the petition. Interested parties will be notified of the grant of any review. For any questions regarding your petition, please contact Airspace Regulations & ATC Procedures Group via telephone -- 202-267-8783 - or facsimile 202-267-9328.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

This aeronautical study considered and analyzed the impact on existing and proposed arrival, departure, and en route procedures for aircraft operating under both visual flight rules and instrument flight rules; the impact on all existing and planned public-use airports, military airports and aeronautical facilities; and the cumulative impact resulting from the studied structure when combined with the impact of other existing or proposed structures. The study disclosed that the described structure would have no substantial adverse effect on air navigation.

An account of the study findings, aeronautical objections received by the FAA during the study (if any), and the basis for the FAA's decision in this matter can be found on the following page(s).

A copy of this determination will be forwarded to the Federal Communications Commission (FCC) because the structure is subject to their licensing authority.

If we can be of further assistance, please contact Karen McDonald, at (310) 725-6557. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2013-AWP-7273-OE.

**Signature Control No: 202110015-208050179**

( DNH )

John Page

Manager, Obstruction Evaluation Group

Attachment(s)

Additional Information

Case Description

Frequency Data

Map(s)

cc: FCC

## Additional information for ASN 2013-AWP-7273-OE

The submitted proposal from Verizon Wireless will construct a 48-foot above ground level (agl) monopole with no appurtenances, in Palm Springs, California.

This site is approximately 1.26 nautical miles northwest of the Palm Springs International (PSP) airport reference point; 2,520 feet direct distance from the Runway 13R physical approach end, the closest civilian public-use landing area.

The PSP Field Elevation (FE) is 477 feet above mean sea level (amsl); Runway 13R physical approach end elevation is 477 feet amsl. The site elevation of this proposed structure is 504 feet amsl.

The structure height exceeds the obstruction standards of Title 14 Code of Federal Regulations (CFR) Part 77, as follows:

Section 77.17(a)(3) - (TERPS criteria); would penetrate the PSP RWY 31L 40:1 departure obstacle clearance surface (OCS) in the Initial Climb Area (ICA) by 14 feet (less than 35 feet.) Mitigation: The proposed structure height would not require an increase in the existing published departure climb gradient (cg), nor would it require an increase in departure weather minimums. It qualifies as a 'low close-in' obstacle penetration with climb gradient termination altitude 200 feet or less above DER, and upon receipt from the sponsor of the 7460-2 Part 1, a note will be added to the 'Take-off Minimums and (Obstacle) Departure Procedures in the U.S. Terminal Procedures publication.

Existing ORS # 06-022099 and # 06-002579, structures of similar height, are located adjacent to this proposal.

Details of this proposal were not distributed for public aeronautical comment because current internal FAA Obstruction Evaluation policy exempts structures that would exceed only the above-cited Section 77.17(a)(3) standard by 35 feet or less.

FAA airspace evaluation has found that the adverse effect of this structure is known. The structure height does not require a change to any existing instrument published climb gradient or departure weather minimums and would not have a significant adverse effect on the TERPS criteria. FAA evaluation finds that the proposal would not create substantial adverse effect on visual aeronautical operations or lessen the utility of the navigable airspace overlying the site.

Existing obstacles and terrain control the development of future approach and departure Terminal Instrument Procedures for PSP landing area. Therefore, no further attempt to negotiate the structure to a lower height was considered necessary.

This does not affect the right to petition for review determinations regarding structures which exceed the subject obstruction standards.

### AERONAUTICAL STUDY FOR POSSIBLE EFFECT UPON THE OPERATION OF AN AIR NAVIGATION AID:

- At a distance of 4100 feet from transmitter site spurious emissions signal levels from proposed transmitters must be less than -104 dBm in the 108-137, 225-400 MHz frequency bands.

AERONAUTICAL STUDY FOR POSSIBLE INSTRUMENT FLIGHT RULES (IFR) EFFECT DISCLOSED THE FOLLOWING:

- The proposal would have no effect on any existing or proposed IFR arrival/departure routes, operations, or procedures.
- The proposal would have no effect on any existing or proposed IFR en route routes, operations, or procedures.
- The proposal would have no effect on any existing or proposed IFR minimum flight altitudes.

AERONAUTICAL STUDY FOR POSSIBLE VISUAL FLIGHT RULES (VFR) EFFECT DISCLOSED THE FOLLOWING:

- The proposal would have no effect on any existing or proposed VFR arrival or departure routes, operations or procedures.
- The proposal would not conflict with airspace required to conduct normal VFR traffic pattern operations at any known civilian public use or military airports.
- The proposal would not penetrate those altitudes normally considered available to airmen for VFR en route flight.
- The structure shall be appropriately red obstruction lighted to make it more conspicuous to airmen flying in VFR weather conditions at night.

The cumulative impact of the proposed structure, when combined with other existing structures is not considered significant. Study did not disclose any adverse effect on existing or proposed civilian public-use or military airports or navigational facilities. Nor would the proposal affect the capacity of any known existing or planned civilian public-use or military airport.

Therefore, it is determined that the proposed construction would not have a substantial adverse effect on the safe and efficient utilization of the navigable airspace by aircraft or on any air navigation facility and would not be a hazard to air navigation.

This determination, issued in accordance with Part 77, concerns the effect of the proposal on the safe and efficient use of the navigable airspace by aircraft and does not relieve the sponsor of any compliance responsibilities relating to laws, ordinances, or regulations of any Federal, state, or local governmental bodies.

Determinations, which are issued in accordance with Part 77, do not supersede or override any state, county, or local laws, aviation easements, or ordinances, or local zoning maximum heights.

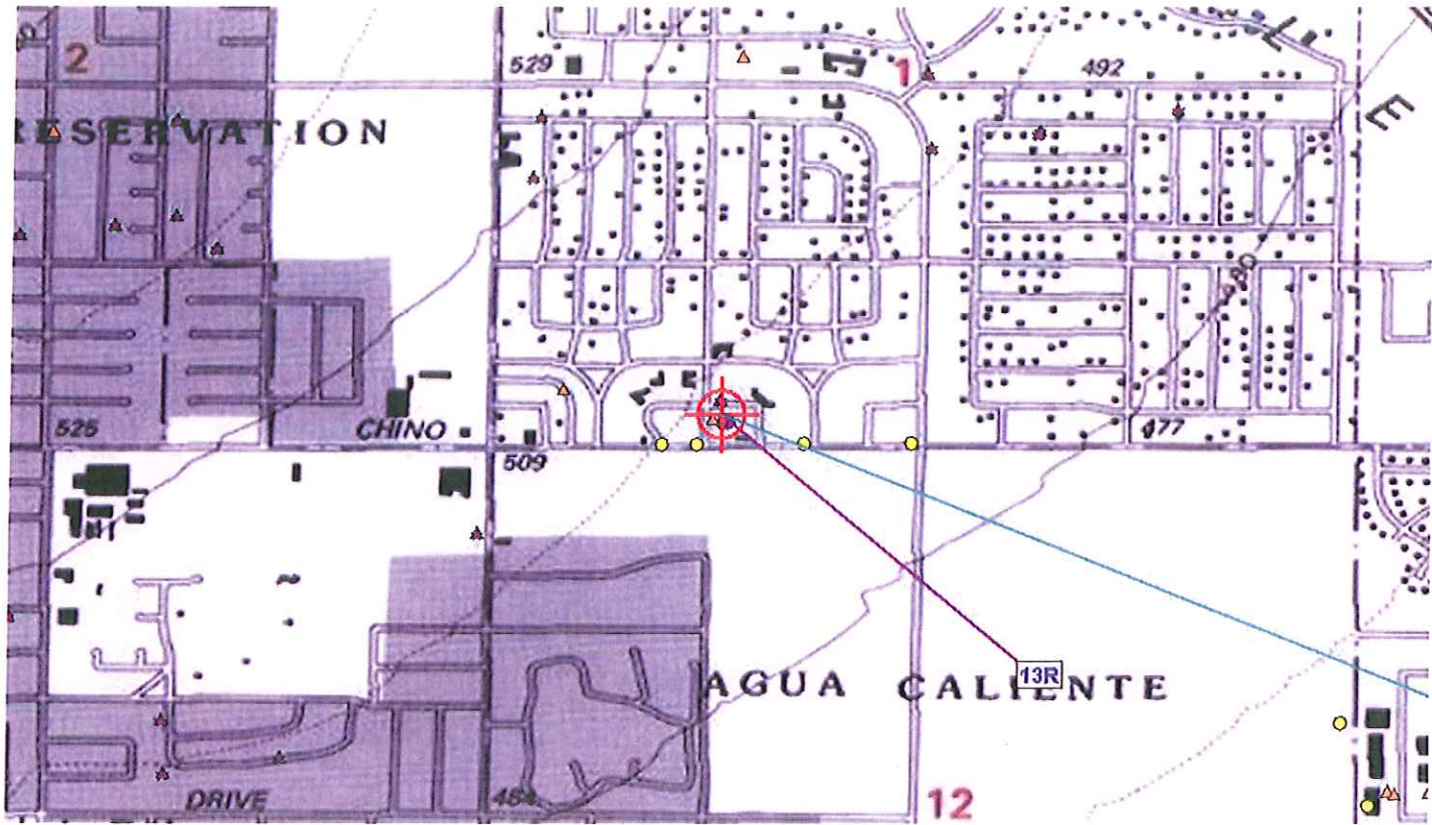


**Case Description for ASN 2013-AWP-7273-OE**

Proposed 48' agl monopole tower. Jim O'Dowd/Verizon Wireless/908-306-7439

Frequency Data for ASN 2013-AWP-7273-OE

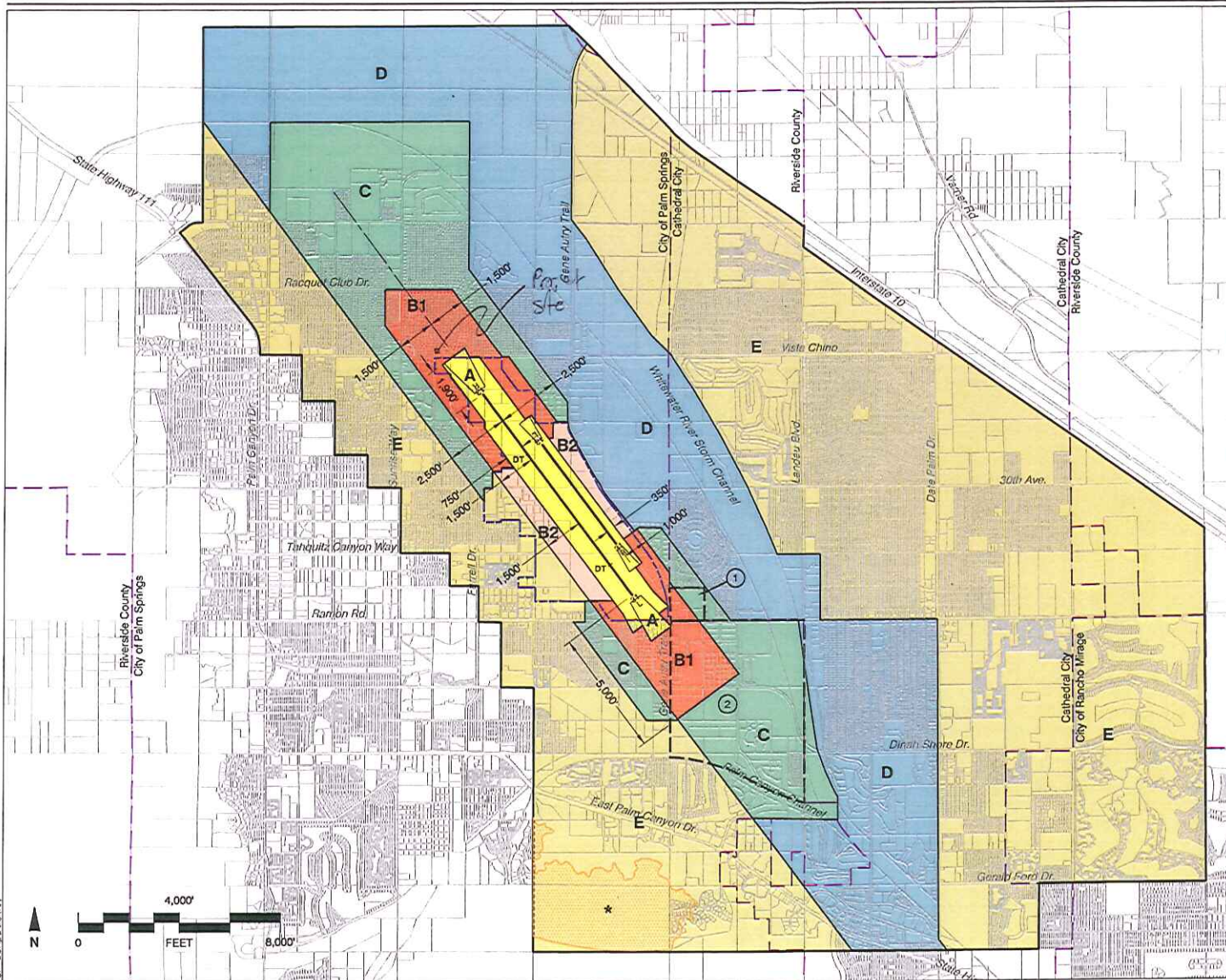
LOW FREQUENCY	HIGH FREQUENCY	FREQUENCY UNIT	ERP	ERP UNIT
698	806	MHz	1000	W
806	824	MHz	500	W
824	849	MHz	500	W
851	866	MHz	500	W
869	894	MHz	500	W
896	901	MHz	500	W
901	902	MHz	7	W
930	931	MHz	3500	W
931	932	MHz	3500	W
932	932.5	MHz	17	dBW
935	940	MHz	1000	W
940	941	MHz	3500	W
1850	1910	MHz	1640	W
1930	1990	MHz	1640	W
2305	2310	MHz	2000	W
2345	2360	MHz	2000	W











**Legend**

- Compatibility Zones**
- Airport Influence Area Boundary
  - Zone A
  - Zone B1
  - Zone B2
  - Zone C
  - Zone D
  - Zone E
  - Height Review Overlay Zone
- Boundary Lines**
- Airport Property Line
  - City Limits

- Notes**
- All dimensions measured from runway ends and centerlines.
  - DT = Displaced Threshold
  - See Chapter 2, Table 2A for compatibility criteria associated with this map.
  - See Policy PS.2.1.

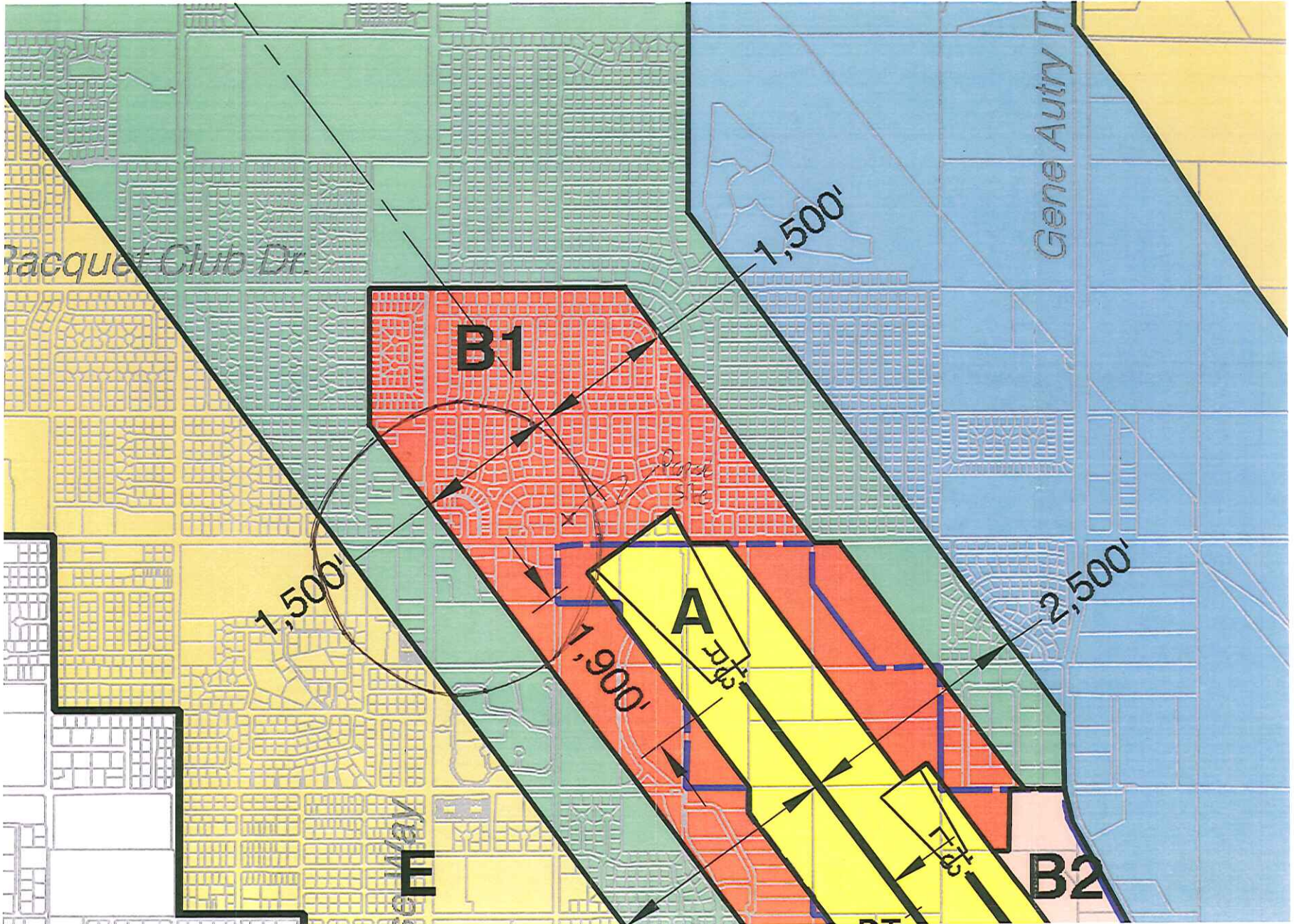
Riverside County  
 Airport Land Use Commission  
 Riverside County  
 Airport Land Use Compatibility Plan  
 Policy Document  
 (Adopted March 2005)

Map PS-1

**Compatibility Map**  
 Palm Springs International Airport

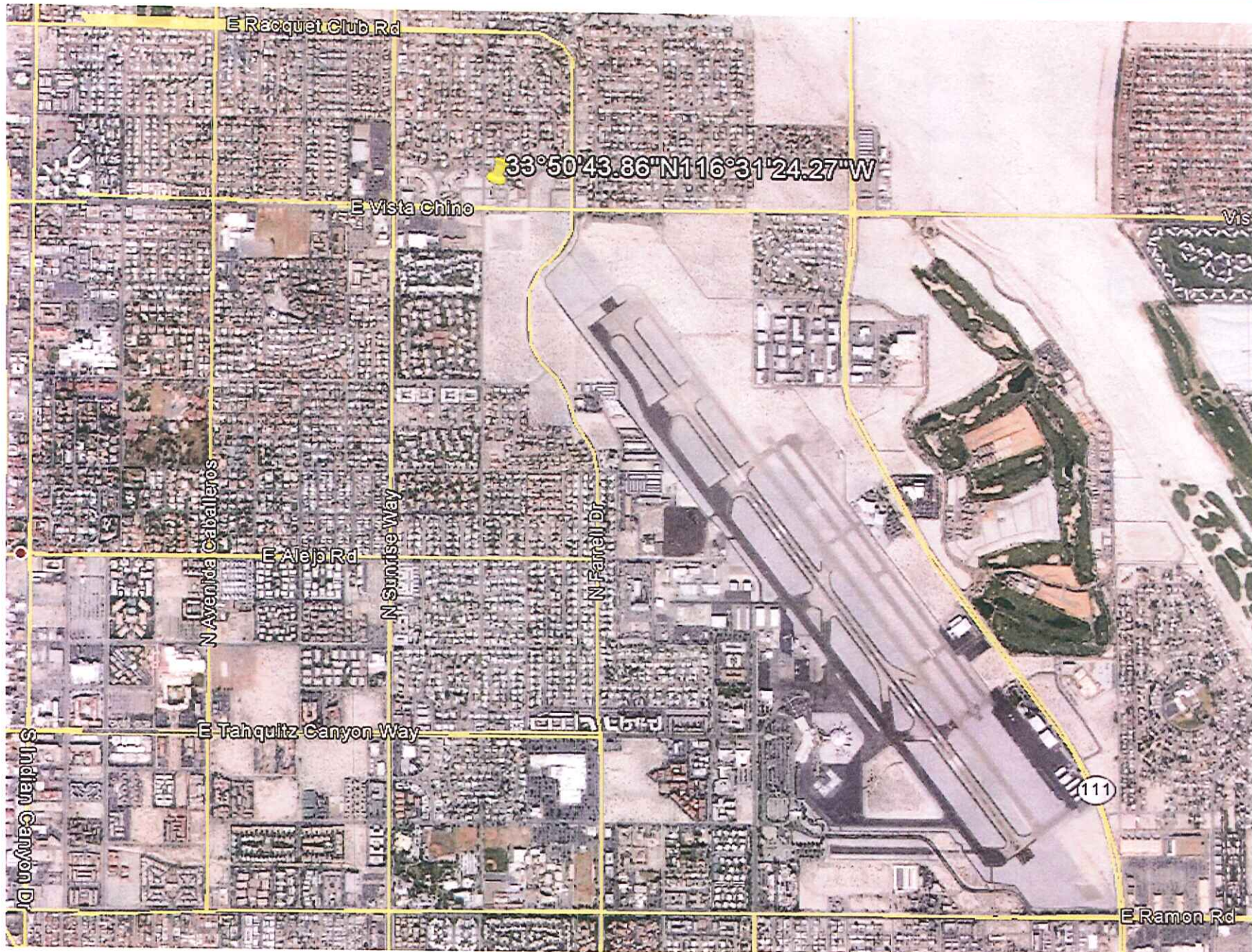
FSP-CORPABILITY





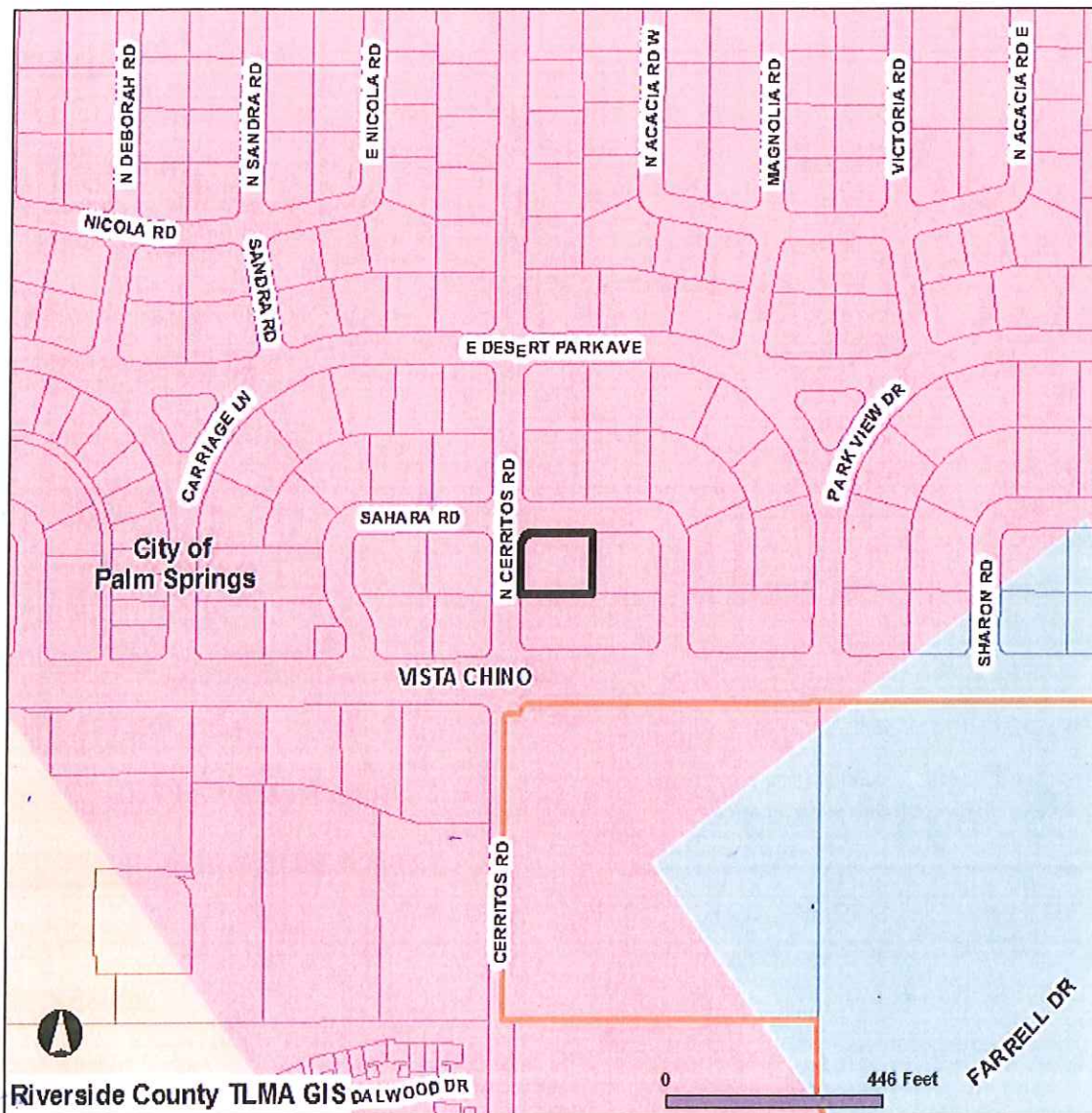


# Project In Relationship to Airport (Desert Park)





RIVERSIDE COUNTY GIS



Selected parcel(s):  
501-351-001

AIRPORTS

- SELECTED PARCEL
- AIRPORT RUNWAYS
- AIRPORT INFLUENCE AREAS
- AIRPORT BOUNDARIES
- INTERSTATES
- HIGHWAYS
- PARCELS
- COMPATIBLTY ZONE A
- COMPATIBLTY ZONE B1
- COMPATIBLTY ZONE C

**\*IMPORTANT\***

Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

STANDARD REPORT

**APNs**

501-351-001-8

**OWNER NAME**

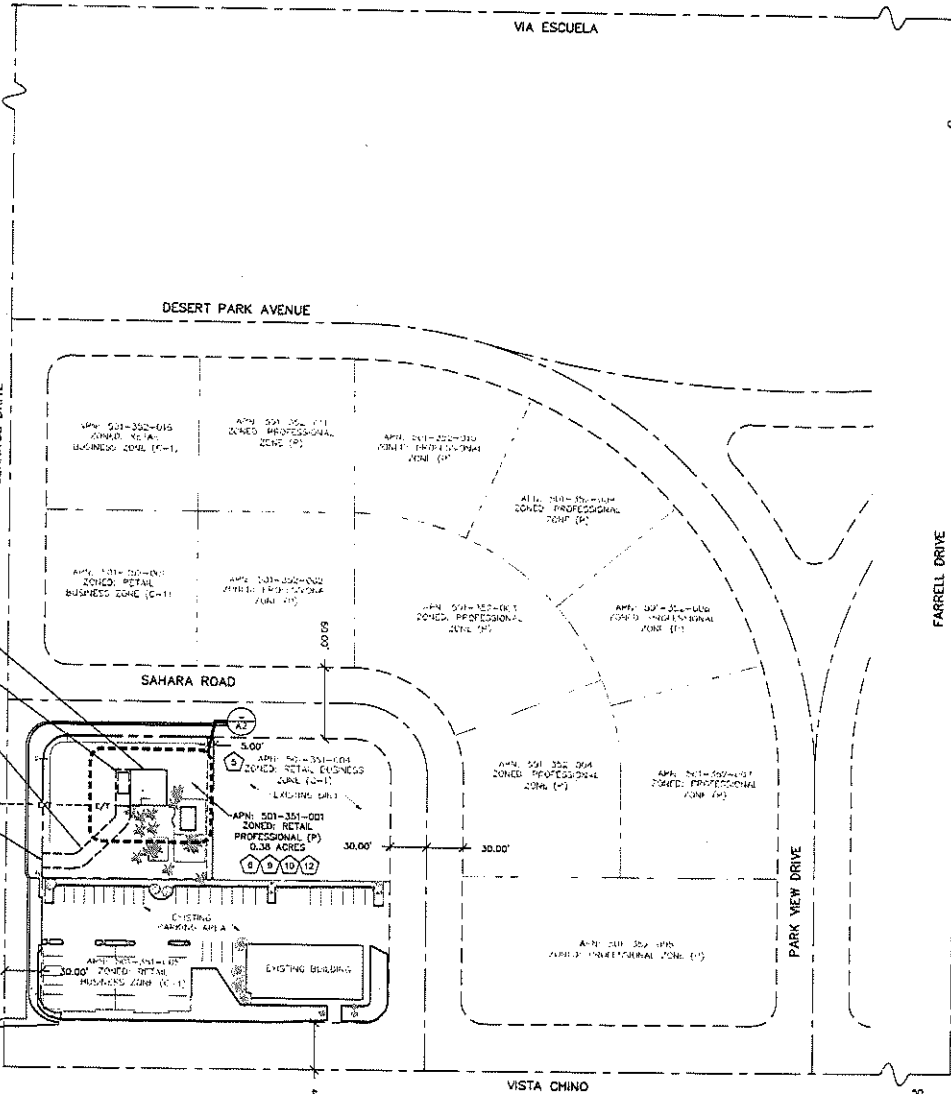
NOT AVAILABLE ONLINE



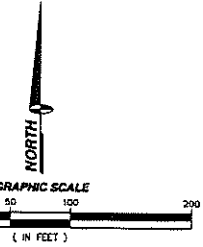
**NOTES & TITLE REPORT EXCEPTIONS**

ONLY THOSE EXCEPTIONS LISTED IN TITLE REPORT ORDER NO. 00193745-990-002, DATED SEPTEMBER 10, 2013, PREPARED BY TIGOR TITLE COMPANY OF CALIFORNIA, WHICH ARE NOT SOLELY FINANCIAL IN NATURE AND WHICH REFERENCE A DOCUMENT CONTAINING A SUFFICIENT LEGAL DESCRIPTION OF AREAS AFFECTED BY SAID DOCUMENT WERE CONSIDERED FOR THIS SURVEY AND RECORDED BELOW. ITEM NUMBERS CORRESPOND TO ITEMS NUMBERS IN SAID TITLE REPORT.

- 3 EASEMENT(S) FOR THE PURPOSE(S) SHOWN BELOW AND RIGHTS INCIDENTAL THEREON AS DEDICATED OR AS OFFERED FOR DEDICATION, ON THE MAP OF SAID TRACT/PLAT. PURPOSE: PUBLIC UTILITIES AFFECTS EASTERLY 5 FEET OF SAID LAND. AFFECTS A PORTION OF THE SUBJECT PROPERTY.
- 4 EASEMENT(S) FOR THE PURPOSE(S) SHOWN BELOW AND RIGHTS INCIDENTAL THEREON, AS GRANTED IN A DOCUMENT. GRANTEE: CALIFORNIA ELECTRIC POWER COMPANY PURPOSE: PUBLIC UTILITIES RECORDING DATE: JULY 20, 1955. RECORDING NO. BOOK 1708, PAGE 477, OF OFFICIAL RECORDS. AFFECTS WITHIN 5 FEET OF ALL DIVISION PROPERTY LINES (EXCEPT SUCH LINES AS COINCIDE WITH BOUNDARY LINES OF STREETS OR ALLEYS) OF SAID TRACT/PLAT. AFFECTS THE ENTIRE SUBJECT PROPERTY.
- 7 COVENANTS, CONDITIONS AND RESTRICTIONS BUT OMITTING ANY COVENANTS OR RESTRICTIONS, IF ANY, INCLUDING BUT NOT LIMITED TO THOSE BASED UPON RACE, COLOR, RELIGION, SEX, SEXUAL ORIENTATION, FAMILIAL STATUS, MARITAL STATUS, DISABILITY, HANDICAP, NATIONAL ORIGIN, ANCESTRY, SOURCE OF INCOME, GENDER, GENDER IDENTITY, GENDER EXPRESSION, MEDICAL CONDITION OR GENETIC INFORMATION, AS SET FORTH IN APPLICABLE STATE OR FEDERAL LAWS, EXCEPT TO THE EXTENT THAT SAID COVENANT OR RESTRICTION IS PERMITTED BY APPLICABLE LAW, AS SET FORTH IN THE DOCUMENT. RECORDING DATE: NOVEMBER 14, 1990. RECORDING NO. BOOK 1737, PAGE 99, OF OFFICIAL RECORDS. SAID COVENANTS, CONDITIONS AND RESTRICTIONS PROVIDE THAT A VIOLATION THEREOF SHALL NOT DEFEAT THE LIEU OF ANY MORTGAGE OR DEED OF TRUST MADE IN GOOD FAITH AND FOR VALUE. SAID COVENANTS, CONDITIONS AND RESTRICTIONS DOES NOT DESCRIBE THE SUBJECT PROPERTY.
- 8 AN UNRECORDED LEASE WITH CERTAIN TERMS, COVENANTS, CONDITIONS AND PROVISIONS SET FORTH THEREIN AS DISCLOSED BY THE DOCUMENT ENTITLED: MEMORANDUM OF AGREEMENT LESSOR: MARYANN J. CORONATE, A MARRIED WOMAN LESSEE: SPRINT PCS ASSETS, L.L.C., A DELAWARE LIMITED LIABILITY COMPANY RECORDING DATE: AUGUST 18, 2004. RECORDING NO. 2004-0649170, OF OFFICIAL RECORDS. THE PRESENT OWNERSHIP OF THE LEASEHOLD CREATED BY SAID LEASE AND OTHER MATTERS AFFECTING THE INTEREST OF THE LESSEE ARE NOT SHOWN HEREIN. AFFECTS A PORTION OF THE SUBJECT PROPERTY, EXACT LOCATION UNDISCLOSED IN DOCUMENT.
- 10 AN UNRECORDED LEASE WITH CERTAIN TERMS, COVENANTS, CONDITIONS AND PROVISIONS SET FORTH THEREIN AS DISCLOSED BY THE DOCUMENT ENTITLED: MEMORANDUM OF AGREEMENT LESSOR: DOUGLAS C. JONES AND TERENCE B. JONES, HUSBAND AND WIFE AS COMMUNITY PROPERTY WITH RIGHTS OF SURVIVORSHIP LESSOR: ROYAL STREET COMMUNICATION CALIFORNIA, L.L.C., A DELAWARE LIMITED LIABILITY COMPANY RECORDING DATE: NOVEMBER 5, 2009. RECORDING NO. 2009-0075204, OF OFFICIAL RECORDS. THE PRESENT OWNERSHIP OF THE LEASEHOLD CREATED BY SAID LEASE AND OTHER MATTERS AFFECTING THE INTEREST OF THE LESSEE ARE NOT SHOWN HEREIN. AFFECTS THE ENTIRE SUBJECT PROPERTY.
- 11 MATTERS CONTAINED IN THAT CERTAIN DOCUMENT ENTITLED: ENCROACHMENT UCDISC AGREEMENT DATED: SEPTEMBER 15, 2010 EXECUTED BY: ROYAL STREET COMMUNICATIONS CALIFORNIA, L.L.C. A DELAWARE LIMITED LIABILITY COMPANY AND CITY OF PALM SPRINGS, A CALIFORNIA CHARTER CITY RECORDING DATE: NOVEMBER 22, 2010. RECORDING NO. 2010-0581150, OF OFFICIAL RECORDS. REFERENCE IS HEREBY MADE TO SAID DOCUMENT FOR FULL PARTICULARS. DOES NOT AFFECT THE SUBJECT PROPERTY.
- 12 A RIGHT OF FIRST REFUSAL TO PURCHASE SAID LAND WITH CERTAIN TERMS, COVENANTS, CONDITIONS AND PROVISIONS AS SET FORTH IN THE DOCUMENT EXECUTED BY: DOUGLAS C. JONES AND TERENCE B. JONES, HUSBAND AND WIFE AS COMMUNITY PROPERTY WITH RIGHTS OF SURVIVORSHIP AND TOWERCO ASSETS L.L.C. A DELAWARE LIMITED LIABILITY COMPANY RECORDING DATE: MAY 21, 2012. RECORDING NO. 2012-0232181, OF OFFICIAL RECORDS. AFFECTS THE ENTIRE SUBJECT PROPERTY.



- LEGEND**
- LEASE AREA
  - CENTERLINE EASEMENT
  - RIGHT-OF-WAY
  - SECTION LINE
  - PROPERTY LINE
  - OVERHEAD POWER
  - EXISTING CHAINLINK FENCE
  - PROPOSED CHAINLINK FENCE
  - PROPOSED WROUGHT IRON FENCE
  - EXISTING 1' CONTOUR
  - EXISTING 5' CONTOUR
  - EXISTING BLOCK WALL
  - PROPOSED BLOCK WALL
  - FIRE HYDRANT
  - PARKING LOT AREA LIGHT
  - OFFICIAL RECORD
  - POWER POLE



**verizon wireless**  
15505 SAND CANYON AVENUE  
BUILDING D, 1ST FLOOR  
IRVINE, CALIFORNIA 92618

**PROJECT INFORMATION:**  
**DESERT PARK (JONES)**  
UNADDRESSSED  
APN: 501-351-001  
PALM SPRINGS, CALIFORNIA  
92262  
**CURRENT ISSUE DATE:**  
10/31/13

**ISSUED FOR:**  
**ZONING**

**REV: DATE DESCRIPTION BY**

REV	DATE	DESCRIPTION	BY
1	10/31/13	100K ZONING	T.R.
2	10/24/13	BOX ZONING	C.P.

**PLANS PREPARED BY:**  
**SPECTRUM**  
SPECTRUM SERVICES, INC.  
8390 MAPLE PLACE, SUITE 110  
RANCHO CUCAMONGA, CA 91730  
PH. (866) 515-3358  
FAX (866) 515-3350

**LICENSURE:**

**SHEET TITLE:**  
**SITE PLAN**

**SHEET NUMBER:** **A1** **REVISION:** **1**

**SITE PLAN**

11" x 17" SCALE (24" x 36" SCALE)  
1" = 100' 1" = 50' 1

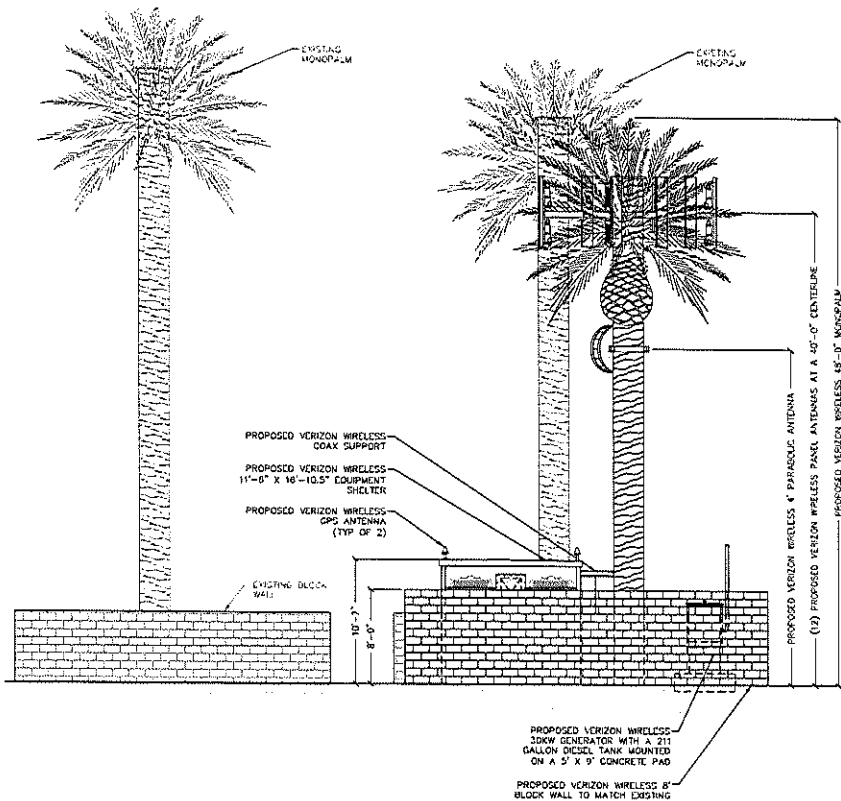
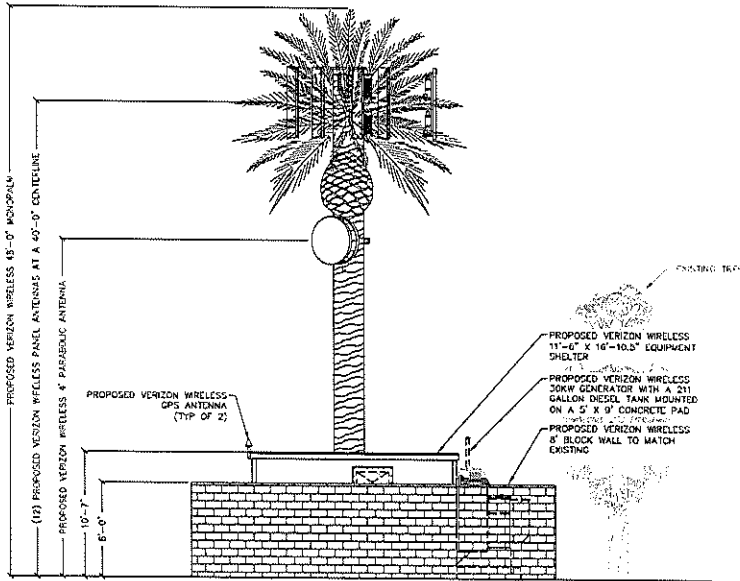
LAX-324





NOTE:  
 1. ALL TOWER MOUNTED APPARATUS SHALL BE PAINTED TO MATCH THE PROPOSED MONOPALM.  
 2. CARRIER COMPOUND IN FOREGROUND, NOT SHOWN FOR CLARITY.

NOTE:  
 1. ALL TOWER MOUNTED APPARATUS SHALL BE PAINTED TO MATCH THE PROPOSED MONOPALM.



EAST ELEVATION

1 1/4" X 1/4" SCALE (24" X 36" SCALE)  
 1" = 10'      1" = 5'

2 NORTH ELEVATION

1 1/4" X 1/4" SCALE (24" X 36" SCALE)  
 1" = 10'      1" = 5'



PROJECT INFORMATION:  
**DESERT PARK (JONES)**  
 UNADDRESSED  
 APN: SD1-351-001  
 PALM SPRINGS, CALIFORNIA  
 92262

CURRENT ISSUE DATE:  
 10/31/13

ISSUED FOR:  
**ZONING**

REV.	DATE	DESCRIPTION	BY

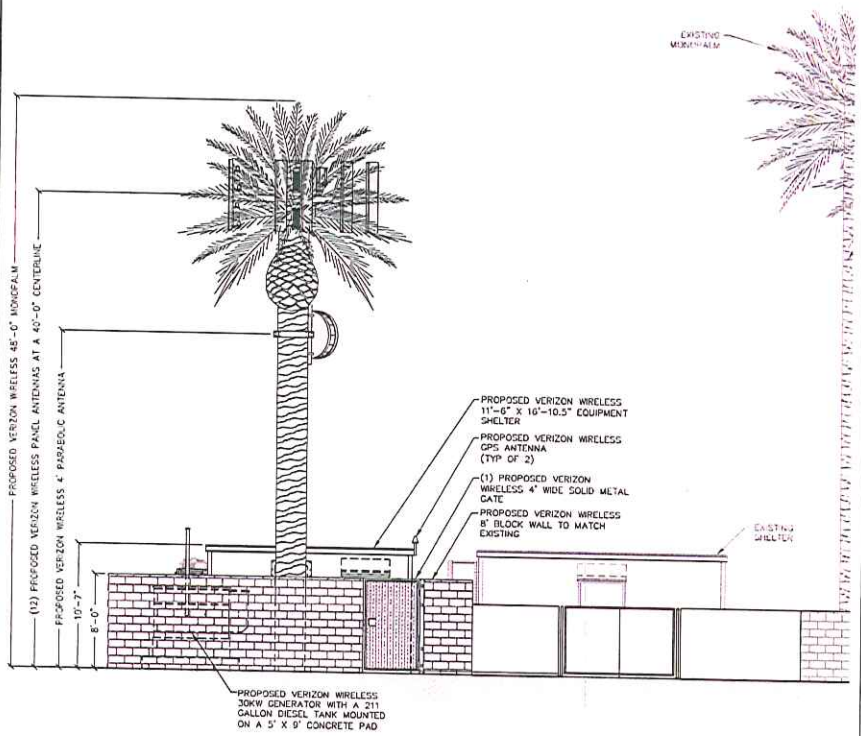
PLANS PREPARED BY:  
**SPECTRUM**  
 SPECTRUM SERVICES, INC.  
 8390 MAPLE PLACE, SUITE 110  
 RANCHO CUCAMONGA, CA 91730  
 PH. (866) 515-3358  
 FAX (866) 515-3359

LICENSURE:  
 SHEET TITLE:  
**NORTH & EAST ELEVATIONS**

SHEET NUMBER:      REVISION:  
**A3**      **1**

LAX-324

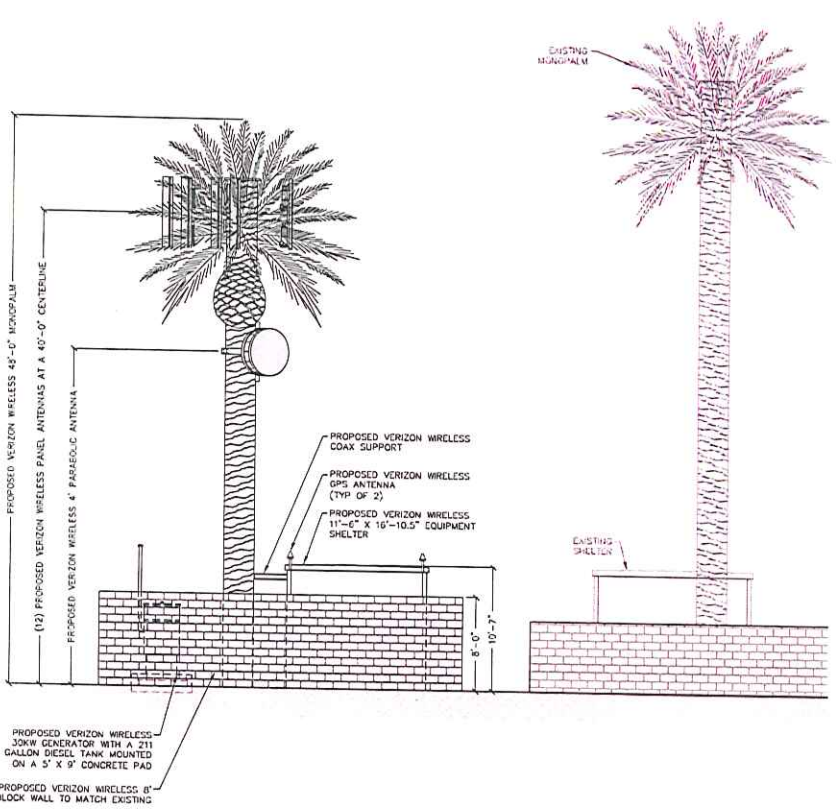
NOTE:  
1. ALL TOWER MOUNTED APPARATUS SHALL BE PAINTED TO MATCH THE PROPOSED MONOPALM.



WEST ELEVATION

11" x 17" SCALE 24" x 36" SCALE  
1" = 10' 1" = 5'

NOTE:  
1. ALL TOWER MOUNTED APPARATUS SHALL BE PAINTED TO MATCH THE PROPOSED MONOPALM.



SOUTH ELEVATION

11" x 17" SCALE 24" x 36" SCALE  
1" = 10' 1" = 5'



PROJECT INFORMATION:  
**DESERT PARK (JONES)**  
UNADDRESSED  
APN: 531-351-001  
PALM SPRINGS, CALIFORNIA 92262

CURRENT ISSUE DATE:  
10/31/13

ISSUED FOR:  
**ZONING**

REV. DATE DESCRIPTION BY:

10/31/13	100R ZONING	T.R.
10/24/13	100R ZONING	C.P.

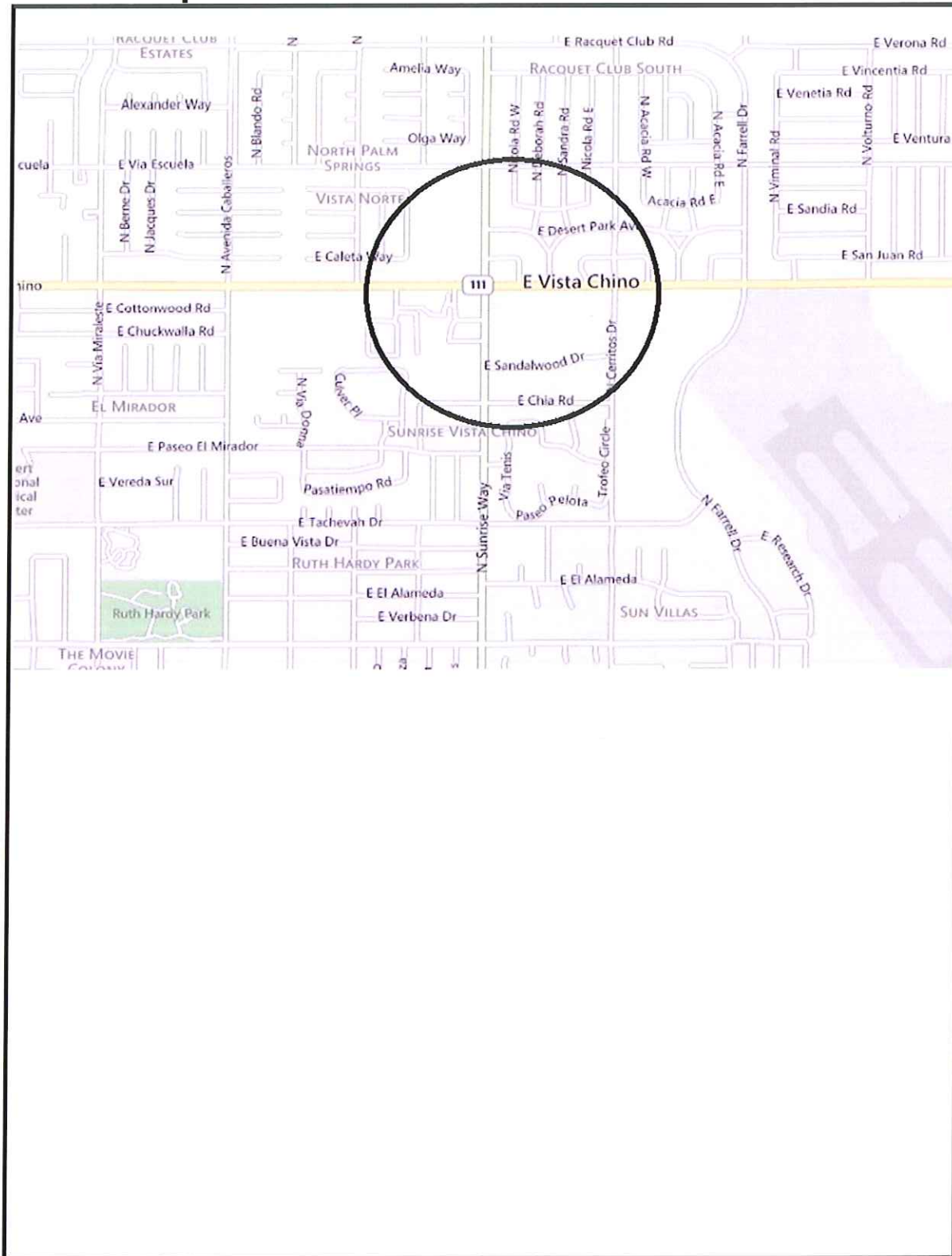
PLANS PREPARED BY:  
**SPECTRUM**  
SPECTRUM SERVICES, INC.  
8390 MAPLE PLACE, SUITE 110  
RANCHO CUCAMONGA, CA 91730  
PH. (866) 515-3358  
FAX (866) 515-3359

LICENSURE:

SHEET TITLE:  
**SOUTH & WEST ELEVATIONS**

SHEET NUMBER: **A4** REVISION: **1**  
LAX-324

# SARF Map





Without Desert Park

DEVELOP  
DEL SOL PCS

AMICO PCS

VISTA CHINO  
VISTA CANINO PCS

Desert Park

Session: Main\_MTX\_44\_45  
 User: mponson  
 Start: Mar 30 15:07:02 2014  
 Default: Square  
 Datum: NAD83  
 Center Lat: 33-50-47.14 N  
 Center Lon: 116-31-26.48 W

Cells:  
 Clr: Cell Version  
 ■: 0000  
 Ltr: Cell Name

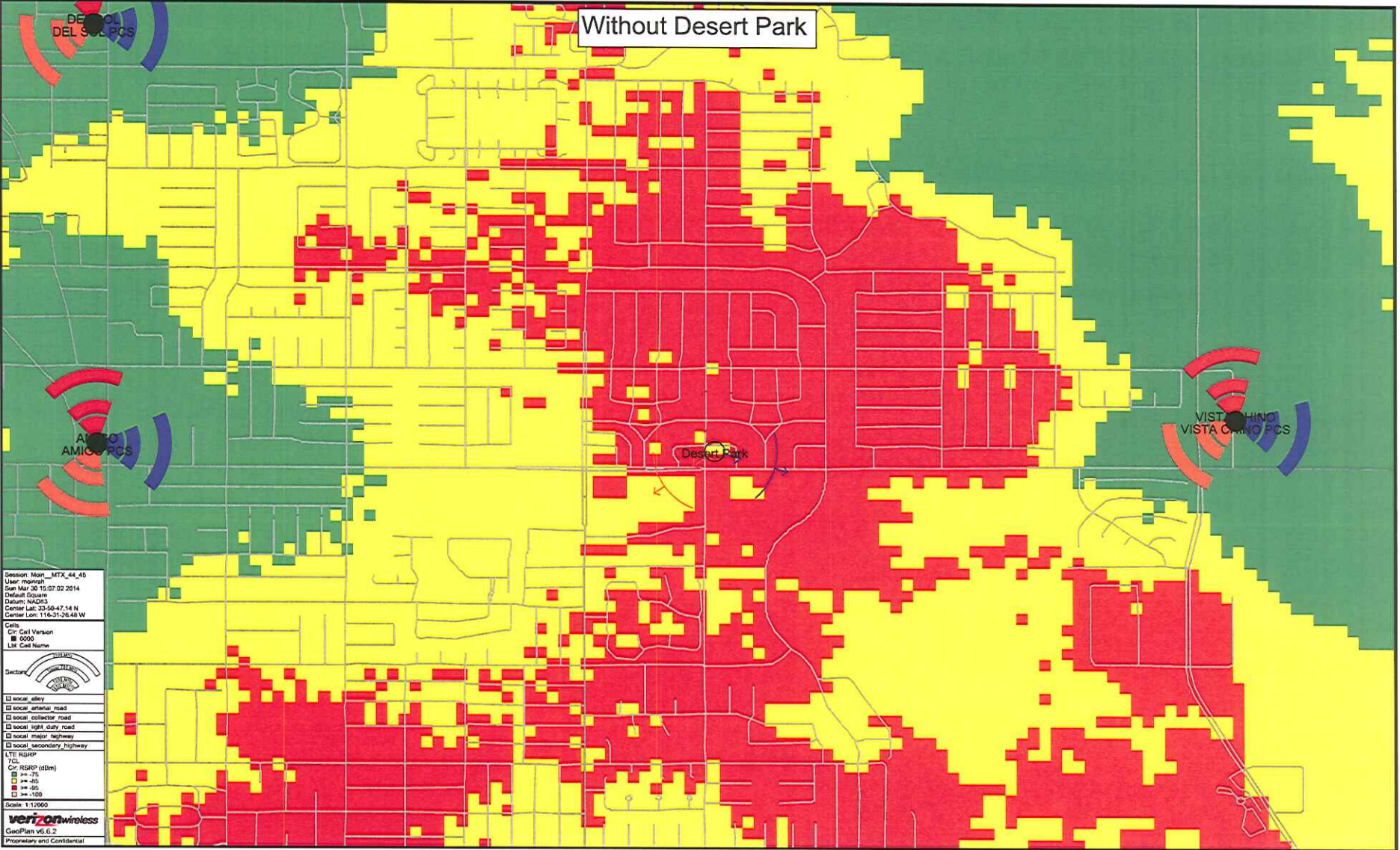
Sectors:  


social\_ally  
 social\_arterial\_road  
 social\_collector\_road  
 social\_light\_duty\_road  
 social\_major\_highway  
 social\_secondary\_highway

LTE RSRP  
 Y/L  
 C/L: RSRP (dBm)  
 ■: >= -75  
 ■: >= -80  
 ■: >= -85  
 ■: >= -100

Scale: 1:12000

**Verizon Wireless**  
 GeoPlan v6.6.2  
 Proprietary and Confidential



Desert Park\_Itself

Desert Park

VISTA CHINO  
VISTA GRANDE RCS

Session Name: MTX\_44\_45  
User: molman  
Fri Mar 21 16:46:06 2014  
Default: Square  
Datum: NAD83  
Center Lat: 33-50-45.81 N  
Center Lon: 116-31-24.25 W

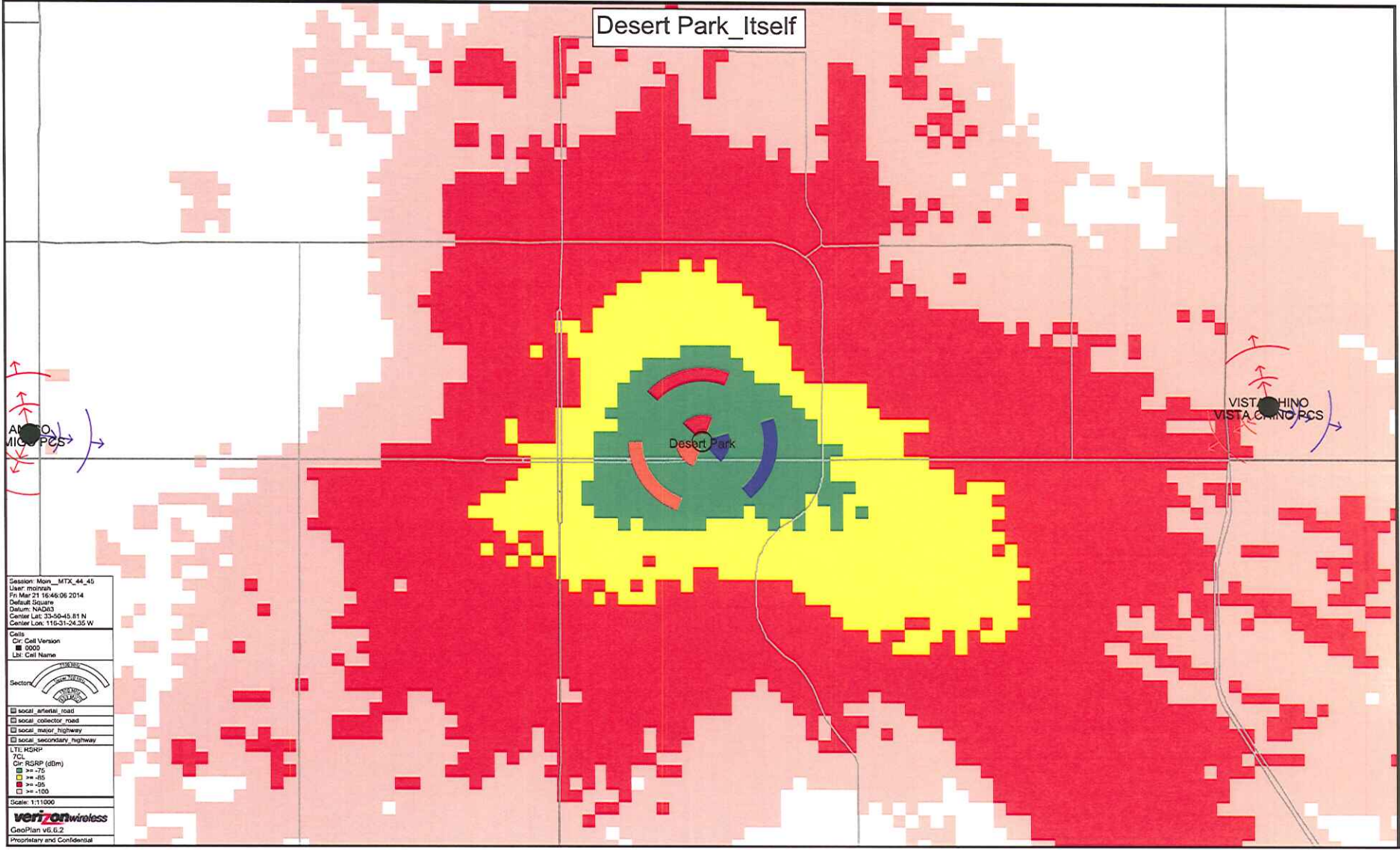
Cells:  
Cell Version: 0000  
Cell Name:



- social\_arterial\_road
- social\_collector\_road
- social\_major\_highway
- social\_secondary\_highway

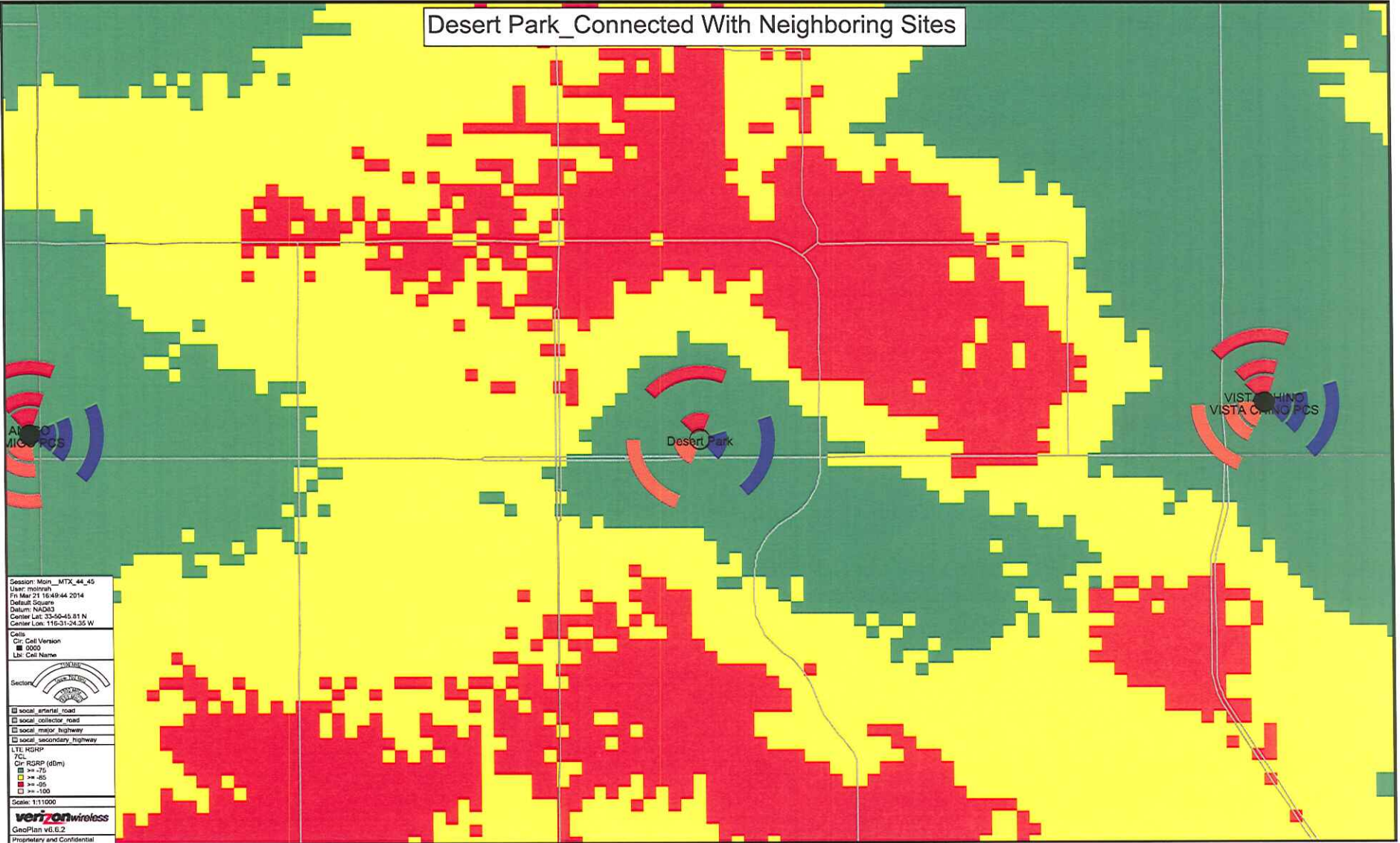
7C: RSRP (dBm)  
Cell Version: 0000  
Cell Name:

Scale: 1:11000  
Verizon Wireless  
GeoPlan v6.6.2  
Proprietary and Confidential





# Desert Park\_Connected With Neighboring Sites



# PHOTOGRAPHIC SIMULATION

PROPOSED WIRELESS COMMUNICATIONS FACILITY



## SITE LOCATION MAP

SITE NAME: DESERT PARK  
SITE NUMBER: LAX-324

SITE ADDRESS: UNADDRESSED  
CORNER OF CERRITOS RD. AND  
VISTO CHINO  
PALM SPRINGS, CA 92262

DATE: 11/7/2013

APPLICANT: VERIZON WIRELESS  
15505 SAND CANYON AVE.  
BUILDING D, 1ST FLOOR  
IRVINE, CA 92618

MICHAEL HAYES, PROJECT MGR.  
909-268-3920



2013 ©GOOGLE MAPS

The included Photograph Simulation(s) are intended as visual representations only and should not be used for construction purposes. The materials represented within the included Photograph Simulation(s) are subject to change.





EXISTING -VIEW 1

PHOTOGRAPHIC SIMULATION -  
VIEW 1



PROPOSED INSTALLATION OF 48' MONOPALM WITH ANTENNA ARRAY,  
INCLUDING EQUIPMENT COMPOUND WITH SHELTER, GENERATOR, AND 8' BRICK  
WALL.



EXISTING -VIEW 2

PHOTOGRAPHIC SIMULATION -  
VIEW 2



PROPOSED INSTALLATION OF 48' MONOPALM WITH ANTENNA ARRAY,  
INCLUDING EQUIPMENT COMPOUND WITH SHELTER, GENERATOR, AND 8' BRICK  
WALL.

## 4.2. Safety

- 4.2.1. *Policy Objective:* The intent of land use safety compatibility criteria is to minimize the risks associated with an off-airport aircraft accident or emergency landing.
- (a) Risks both to people and property in the vicinity of an airport and to people on board the aircraft shall be considered.
  - (b) The most stringent land use controls shall be applied to the areas with the greatest potential risks.
- 4.2.2. *Risks to People on the Ground:* The principal means of reducing risks to people on the ground is to restrict land uses so as to limit the number of people who might gather in areas most susceptible to aircraft accidents. The usage intensity criteria cited in Table 2A reflect the risks associated with various locations in the environs of the airports in the county. (Methods for determining the concentration of people for various land uses are provided in Appendix C.)
- 4.2.3. *Land Uses of Special Concern:* Certain types of land uses represent special safety concerns irrespective of the number of people associated with those uses. Land uses of particular concern include:
- (a) *Uses Having Vulnerable Occupants:* Uses in which the occupants have reduced effective mobility or are unable to respond to emergency situations shall be prohibited within all *Compatibility Zones* except *Zone E*. These uses include children's schools and day care centers (with 7 or more children), hospitals, nursing homes, and other uses in which the majority of occupants are children, elderly, and/or handicapped.
    - (1) This general policy may be superseded by airport specific policies (see Chapter 3).
    - (2) Hospitals are medical facilities which include provision for overnight stays by patients. Medical clinics are permitted in *Compatibility Zones C* and *D* provided that these facilities meet the maximum intensity standards listed in the Compatibility Criteria matrix, Table 2A.
  - (b) *Multi-story Buildings:* In the event of an emergency resulting from an aircraft accident, low-rise buildings can be more readily evacuated than those with more floors. On this basis, the following limitations are established:
    - (1) Within *Compatibility Zone A*, new occupied structures are not permitted.
    - (2) Within *Compatibility Zones B1* and *B2*, new buildings shall be limited to no more than two occupied floors above ground.
    - (3) Within *Compatibility Zone C*, new buildings shall be limited to no more than three occupied floors above ground.
  - (c) *Hazardous Materials Storage:* Construction of facilities for the manufacture or storage of fuel, explosives, and other hazardous materials within the airport environs is restricted as follows:
    - (1) Within *Compatibility Zone A*, manufacture or storage of any such substance is prohibited.
    - (2) Within *Compatibility Zones B1* and *B2*, only the following is permitted:
      - › Fuel or hazardous substances stored in underground tanks.

- › On-airport storage of aviation fuel and other aviation-related flammable materials.
  - › Aboveground storage of less than 6,000 gallons of nonaviation flammable materials (this limit coincides with a break-point used in the Uniform Fire Code to distinguish between different classes of tanks).
- (3) Within *Compatibility Zone C*, manufacture or storage of hazardous materials other than the types listed in Sub-policy (2) above is prohibited unless no other feasible alternative site exists and the facility is designed in a manner that minimizes its susceptibility to damage from an aircraft accident.
- (d) Critical Community Infrastructure: Construction of power plants, electrical substations, public communications facilities, and other critical community infrastructure shall be restricted as follows:
- (1) Within *Compatibility Zone A*, all such uses are prohibited.
  - (2) Within *Compatibility Zones B1* and *B2*, such uses are prohibited unless no other feasible alternative site exists and the facility is designed in a manner that minimizes its susceptibility to damage from an aircraft accident.
- 4.2.4. *Open Land*: In the event that a light aircraft is forced to land away from an airport, the risks to the people on board can best be minimized by providing as much open land area as possible within the airport vicinity. This concept is based upon the fact that the majority of light aircraft accidents and incidents occurring away from an airport runway are controlled emergency landings in which the pilot has reasonable opportunity to select the landing site.
- (a) To qualify as open land, an area should be:
    - (1) Free of most structures and other major obstacles such as walls, large trees or poles (greater than 4 inches in diameter, measured 4 feet above the ground), and overhead wires.
    - (2) Have minimum dimensions of approximately 75 feet by 300 feet.
  - (b) Roads and automobile parking lots are acceptable as open land areas if they meet the above criteria.
  - (c) Open land requirements for each compatibility zone are to be applied with respect to the entire zone. Individual parcels may be too small to accommodate the minimum-size open area requirement. Consequently, the identification of open land areas must initially be accomplished at the general plan or specific plan level or as part of large (10 acres or more) development projects.
  - (d) Clustering of development, subject to the limitations noted below, and providing contiguous landscaped and parking areas is encouraged as a means of increasing the size of open land areas.
  - (e) Building envelopes and the airport compatibility zones should be indicated on all development plans and tentative maps for projects located within the influence area of airports covered by this *Compatibility Plan*. Portraying this information is intended to assure that individual development projects provide the open land areas identified in the applicable general plan, specific plan, or other large-scale plan.

## Guerin, John

---

**From:** Bill Ferra <rezwefttet@gmail.com>  
**Sent:** Monday, March 31, 2014 10:05 AM  
**To:** Guerin, John  
**Subject:** ZAP1020PS14 -cell/antennas on 48ft monopalm tower, southeast corner Sahara Rd. and N Cerritos Rd in Palm Springs

John Guerin - Per our discussion today, we are in favor of the additional cell tower at the above referenced location. We live very close and can see this property unobstructed and endorse this additional cell tower. The maintenance has been very consistent and good as far as the property is concerned. We assume the additional tax revenues will help the city of Palm Springs and therefore we would hope the city would approved the additional tower... we would normally attend these meetings as in the past they were held in the city of Palm Springs, but we have a prior commitment making it impossible for us to attend. John thank you for your attention in this matter, William Ferra and Ron Zimmerman 2101 E. Desert Park Ave. Palm Springs, Ca. 92262.



This email is free from viruses and malware because [avast! Antivirus](#) protection is active.



# NOTICE OF PUBLIC HEARING

## RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14<sup>th</sup> Floor, Riverside, California 92501, Monday through Thursday from 8:00 a.m. to 5:00 p.m., and by appointment on Fridays from 8:30 a.m. to 4:30 p.m.

PLACE OF HEARING: Riverside County Administration Center  
4080 Lemon St., 1<sup>st</sup> Floor Hearing Room  
Riverside, California

DATE OF HEARING: April 10, 2014

TIME OF HEARING: 9:00 A.M.

### CASE DESCRIPTION:

ZAP1020PS14 – Spectrum Services, Inc. (Representative: Brett Smirl/Michael Hayes) - City of Palm Springs Case Nos. CUP 5.1314 (Conditional Use Permit) and VAR 6.530 (Variance). CUP 5.1314 is a proposal to establish an unmanned telecommunications facility consisting of antennas on a 48-foot high monopalm tower, with associated equipment shelter, on a 900 square foot lease area within a 0.39-acre parcel located at the southeasterly corner of Sahara Road and (North) Cerritos Road in the City of Palm Springs. VAR 6.530 is a proposal to allow the 48-foot high structure in the P (Professional) Zone. Without a variance, the allowable height limit for antennas is 15 feet. (Zone B1 of the Palm Springs International Airport Influence Area).

FURTHER INFORMATION: Contact Russell Brady at (951) 955-0549 or John Guerin at (951) 955-0982. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Mr. David Newell of the City of Palm Springs Planning Department, at (760) 323-8245.

Disc. of Park

**APPLICATION FOR MAJOR LAND USE ACTION REVIEW**  
**RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**

ALUC Identification No.

ZAP1020PS14

**PROJECT PROPONENT (TO BE COMPLETED BY APPLICANT)**

Date of Application

Property Owner

Doug Jones

Phone Number (760) 275-4979

Mailing Address

3500 E. Tachevah Dr.  
Palm Springs CA 92262

Agent (if any)

Spectrum Svcs / Brett Smith

Phone Number (909) 944-5471

Mailing Address

8390 Maple Pl #110  
Rancho Cucamonga CA 91730

X 20

**PROJECT LOCATION (TO BE COMPLETED BY APPLICANT)**

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address

Unaddressed Parcel

Assessor's Parcel No.

501-351-001

Parcel Size

Subdivision Name

Zoning

Lot Number

Classification

**PROJECT DESCRIPTION (TO BE COMPLETED BY APPLICANT)**

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe)

Residential

Proposed Land Use (describe)

Unmanned telecommunications Facility.  
48' monopalm. The equip. to be located inside a shelter with the generator in a proposed 6' black wall enclosure.

For Residential Uses

Number of Parcels or Units on Site (exclude secondary units)

One

For Other Land Uses

Hours of Use

24 hours / 7 days per week

(See Appendix C)

Number of People on Site

Maximum Number

Unmanned

Method of Calculation

Height Data

Height above Ground or Tallest Object (including antennas and trees)

48

ft.

Highest Elevation (above sea level) of Any Object or Terrain on Site

ft.

Flight Hazards

Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?

Yes

No

If yes, describe

REFERRING AGENCY (APPLICANT OR JURISDICTION TO COMPLETE)	
Date Received	2-27-14
Agency Name	City of Palm Springs
Staff Contact	
Phone Number	
Agency's Project No.	CUP 5-1314 Var 6.530
Type of Project	<input type="checkbox"/> General Plan Amendment <input type="checkbox"/> Zoning Amendment or Variance <input type="checkbox"/> Subdivision Approval <input type="checkbox"/> Use Permit <input type="checkbox"/> Public Facility <input type="checkbox"/> Other _____

A. **NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. **SUBMISSION PACKAGE:**

**ALUC REVIEW**

- 1 . . . . . Completed Application Form
- 1 . . . . . Project Site Plan – Folded (8-1/2 x 14 max.)
- 1 . . . . . Elevations of Buildings - Folded
- 1 Each . 8 ½ x 11 reduced copy of the above
- 1 . . . . . 8 ½ x 11 reduced copy showing project in relationship to airport.
- 1 Set . Floor plans for non-residential projects
- 4 Sets . . Gummed address labels of the Owner and representative (*See Proponent*).
- 1 Set . . Gummed address labels of all property owners within a 300' radius of the project site. If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10), with ALUC return address.
- 4 Sets . . Gummed address labels of the referring agency (City or County).
- 1 . . . . . Check for Fee (See Item "C" below)

**STAFF REVIEW (Consult with ALUC staff planner as to whether project qualifies)**

- 1 . . . . . Completed Application Form
- 1 . . . . . Project Site Plans – Folded (8-1/2 x 14 max.)
- 1 . . . . . Elevations of Buildings - Folded
- 1 . . . . . 8 ½ x 11 Vicinity Map
- 1 Set . Gummed address labels of the Owner and representative (*See Proponent*).
- 1 Set . . Gummed address labels of the referring agency.
- 1 . . . . . Check for review--See Below



# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

## STAFF REPORT

### ADMINISTRATIVE ITEMS

- 3.1 Director's Approvals. During the month of March, as authorized pursuant to Section 1.5.2(d) of the 2004 Riverside County Airport Land Use Compatibility Plan, ALUC Director Ed Cooper reviewed three non-legislative cases in the Coachella Valley and determined that the proposals were consistent with the applicable Compatibility Plans.

ZAP1021TH14 (Plot Plan No. 25436, unincorporated County area) proposes establishment of interim/temporary facilities at Thermal Motorsports Park (a project previously reviewed by the Commission), including a 4,732 square foot Interim Sales Trailer/Club Facility, an 8,572 square foot Interim Marketing Facility, 17,692 square foot Interim Track Operation Area, and 6,072 square foot Interim Driving Instruction Area. (The project is located in Airport Compatibility Zones B1, C, and D of the Jacqueline Cochran Regional Airport Influence Area.)

ZAP1019PS14 (Conditional Use Permit and Variance, City of Palm Springs Case Nos. 5.0793 CUP and 6.535 VAR) proposes establishment of an unmanned telecommunications facility (antennas on a 43-foot high monopole, with associated equipment shelter) on a 900 square foot lease area within a parcel owned by the City of Palm Springs located southerly of Mesquite Avenue, westerly of Mountain View Drive, and easterly of California Avenue, in Airport Compatibility Zone E of the Palm Springs International Airport Influence Area.

ZAP1054BD14 (Site Development Plan, City of La Quinta Case No. SDP 14-940) proposes single-family residences on each of nineteen lots proposed for recordation through Tract Map No. 31087. The Tract Map was approved in 2004, prior to the adoption of the current Bermuda Dunes Airport Land Use Compatibility Plan. The site is located southerly of Darby Road and easterly of Palm Royale Drive in the City of La Quinta, in Airport Compatibility Zone E of the Bermuda Dunes Airport Influence Area.

Copies of the approval letters and background information are attached, for the Commission's information.

- 3.2 Compatibility Plan Status Update. March ARB - Work continues to progress toward the preparation of the Draft Environmental Impact Report for the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan. Staff has met with representatives of the Planning or Community Development Departments of each of the affected jurisdictions and provided them with copies of the displacement analysis applicable to their respective jurisdictions. As of March 27, the jurisdictions were reviewing this information in order to provide input as to whether they consider the impacts to be significant. The next step will be the preparation (by ESA) of the administrative draft of the Environmental Impact Report for review by ALUC

staff and Gatzke, Dillon, and Ballance. Staff has directed ESA to move forward with preparation of the administrative draft and expects receipt by April 24. The administrative draft will be reviewed by ALUC staff, Counsel, and ALUC's CEQA Counsel (Gatzke, Dillon, and Ballance), with corrections and edits provided to ESA and Mead & Hunt. The consultants will then prepare and distribute the Draft EIR. We hope to begin the 45-day public review period before the end of May.

The Commission has previously indicated a desire to consider major items such as Compatibility Plans and City General Plan reviews over the course of two meetings. If we are able to maintain the above schedule, we would provide for an initial presentation of the Compatibility Plan at the Commission's July meeting. Since this would likely be prior to the close of the EIR public review period, the Commission would not be able to take final action at that time. Following the completion of the public review period, ALUC staff, Counsel, and ALUC's CEQA Counsel would review the comments and work with the consultants in the formulation of Responses to Comments, which would be included in the Final EIR document. It is our anticipation that the Final EIR would be completed in August, so as to allow for the 10-day Final EIR public notice to be issued in sufficient time for the Commission to take final action to certify the EIR and adopt the Plan at its September hearing.

If milestones can be met earlier, there is a possibility that a Final EIR could be completed at an earlier date, which could allow possible adoption at a special meeting in August. Staff will be able to advise the Commission at, or prior to, its June meeting if this emerges as a possibility.

Banning Municipal – The Banning City Council has apparently decided not to move forward with providing an allocation of up to \$25,000 for the preparation of an amendment to the non-residential intensity criteria of Zone D, as applicable within the Banning Municipal Airport Influence Area. A more comprehensive amendment to the Banning Municipal Airport Land Use Compatibility Plan remains a high priority for this Commission.

Hemet-Ryan Airport – Staff has invited Economic Development Agency – Aviation Division staff to provide an update to the Commission on possible Airport Layout Plan options for Hemet-Ryan Airport.

- 3.3 2014 California Airport Land Use Consortium Conference – The 2014 Conference was held in Rohnert Park in Sonoma County. Mead & Hunt joined the San Joaquin Council of Governments as conference sponsors this year, with Ken Brody serving as the primary emcee. Chairman Simon Housman served as a moderator on one panel and a speaker on another. John Guerin was assigned to the first panel addressing the preparation of Airport Land Use Compatibility Plans and provided a presentation regarding guidance from the California Airport Land Use Planning Handbook. Commissioner John Lyon and ALUC Counsel Anna Wang also attended.



# AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

March 5, 2014

**CHAIR**  
Simon Housman  
Rancho Mirage

**VICE CHAIRMAN**  
Rod Ballance  
Riverside

**COMMISSIONERS**

Arthur Butler  
Riverside

Glen Holmes  
Hemet

John Lyon  
Riverside

Greg Pettis  
Cathedral City

Richard Stewart  
Moreno Valley

**STAFF**

Director  
Ed Cooper

John Guerin  
Russell Brady  
Barbara Santos

County Administrative Center  
4080 Lemon St., 14<sup>th</sup> Floor.  
Riverside, CA 92501  
(951) 955-5132

[www.rcaluc.org](http://www.rcaluc.org)

Mr. Jay Olivas, Urban Regional Planner IV  
County of Riverside Planning Department  
4080 Lemon Street, 12<sup>th</sup> Floor  
Riverside, CA 92501  
**HAND DELIVERY**

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW**

File No.: ZAP1021TH14  
Related File No.: PP25436 (Plot Plan)  
APN: 759-180-004; 759-180-005; 759-180-008; 759-180-013

Dear Mr. Olivas:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC), staff reviewed the above-referenced proposal for temporary facilities associated with the Thermal Motorsports Park, including a 4,732 square foot Interim Sales Trailer/Club Facility, 8,572 square foot Interim Marketing Facilities, 17,692 square foot Interim Track Operation Area, and 6,072 square foot Interim Driving Instruction Area as noted on the site plan provided. In addition, the project has already been conditioned for those facilities within Compatibility Zone B1 to comply with the 50 people per single-acre criteria and those facilities within Compatibility Zone C to comply with the 150 people per single-acre criteria. This review, its finding, and recommended conditions are intended solely for the interim/temporary facilities associated with the Thermal Motorsports Park. All prior reviews, determinations, and recommended conditions are still applicable to permanent and other facilities previously reviewed. The site is located within Airport Compatibility Zones B1, C, and D of the Jacqueline Cochran Regional Airport Influence Area (AIA).

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2005 Jacqueline Cochran Regional Airport Land Use Compatibility Plan (amended 2006), subject to the following conditions:

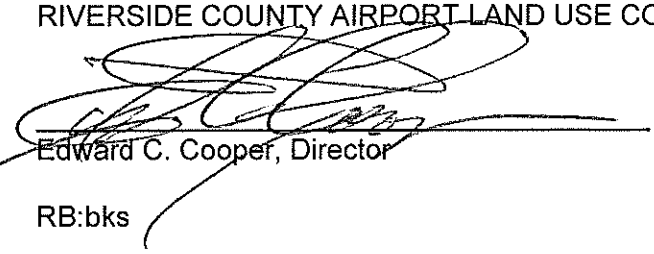
**CONDITIONS:**

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky, and shall comply with Riverside County Ordinance No. 655, as applicable. Outdoor lighting plans, if any, shall be transmitted to Riverside County Economic Development Agency – Aviation Division personnel and to the Jacqueline Cochran Regional Airport for review and comment. (Failure to comment within thirty days shall be considered to constitute acceptability on the part of the airport manager.)
2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations, or any type of strobe light,

- toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
- (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, livestock operations, production of cereal grains, sunflower, and row crops, artificial marshes, wastewater management facilities, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, incinerators, and landfills.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Children's schools, day care centers, libraries, hospitals, nursing homes, places of worship, highly noise-sensitive outdoor nonresidential uses, and aboveground bulk storage of 6,000 gallons or more of hazardous or flammable materials.
3. Any detention or retention basin shall be designed so as to provide a maximum 48-hour detention period for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the retention basin that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
4. Development of the area addressed through Plot Plan No. 25436 shall comply with all nonresidential intensity criteria and open area requirements of the applicable airport compatibility zones.

If you have any questions, please contact Russell Brady, Contract Planner, at (951) 955-0549 or John Guerin, Principal Planner, at (951) 955-0982.

Sincerely,  
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



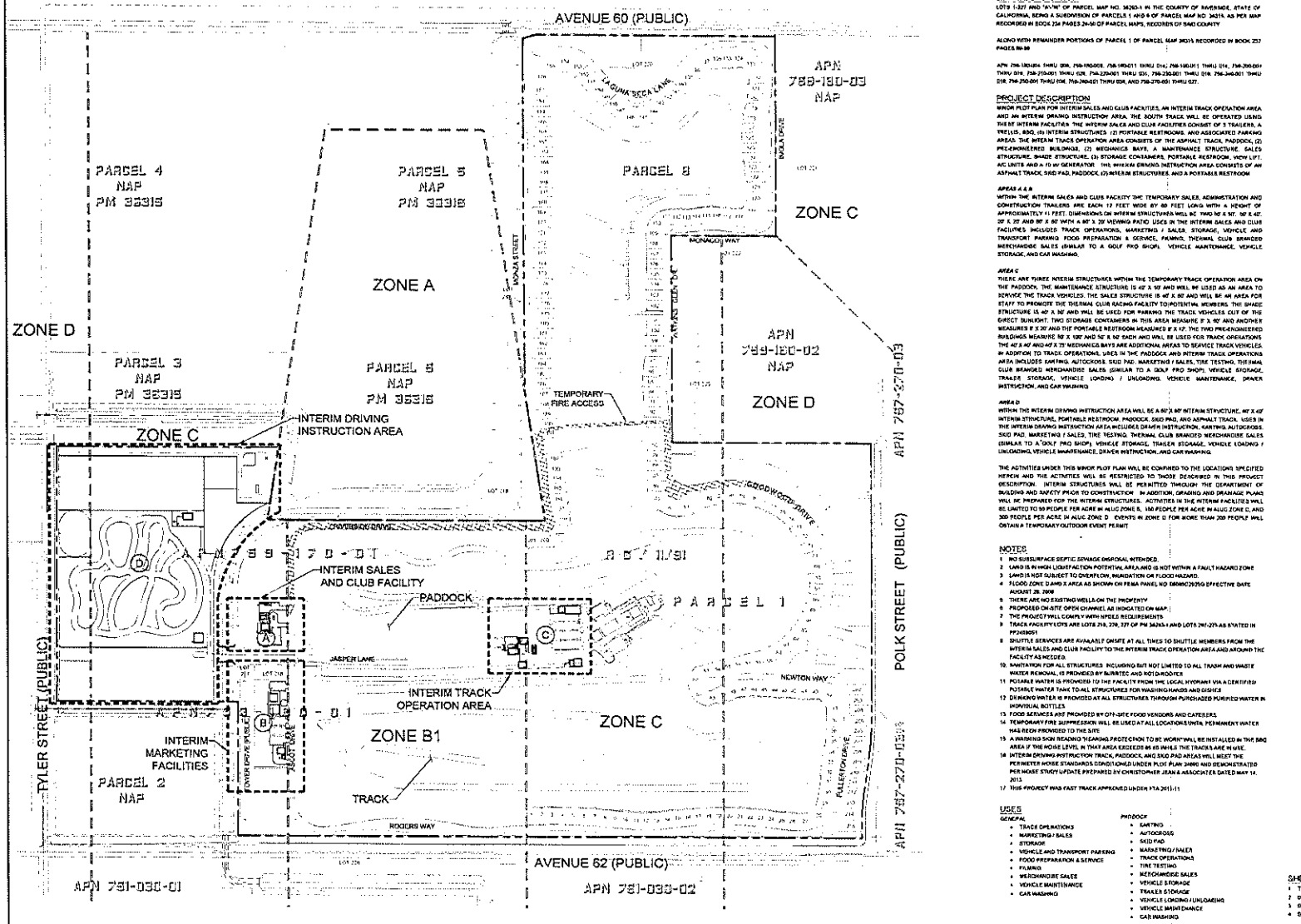
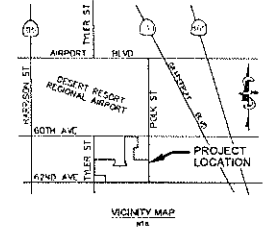
Edward C. Cooper, Director

RB:bks

cc: Tom Collopy, Discovery Land Company LLC  
Tim Rogers, Thermal Operating Company  
Rich Clark, RCE Consultants, Inc.  
JTM Land Company (owner)  
Chad Wilshire, Riverside County EDA – Aviation Division  
ALUC Staff



**MINOR PLOT PLAN NO. 25436 - THE THERMAL CLUB**  
 PARCEL MAP 36293-1 BEING A PORTION OF THE SE 1/4 OF SECTION 33 T.6S, R.8E, S.B.B.M.



**LEGAL DESCRIPTION**  
 LOTS 1327 AND 1427 OF PARCEL MAP NO. 10033 IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, BEING A SUBDIVISION OF PARCELS 1 AND 2 OF PARCEL MAP NO. 24543-1 NOT RECORDED IN BOOK 204 PAGES 24-26 OF PARCEL MAPS, RECORDS OF SAID COUNTY

ALSO WITH REMAINDER PORTIONS OF PARCEL 1 OF PARCEL MAP 3615 RECORDED IN BOOK 222 PAGES 80-82

APN 789-100-03 NAP

APN 789-100-02 NAP

**PROJECT DESCRIPTION**  
 THIS PLOT PLAN FOR INTERIM SALES AND CLUB FACILITIES, AN INTERIM TRACK OPERATION AREA AND AN INTERIM DRIVING INSTRUCTION AREA. THE SOUTH TRACK WILL BE OPERATED USING THESE INTERIM FACILITIES. THE INTERIM SALES AND CLUB FACILITIES CONSIST OF 3 TRAILERS, A TRUCK, AND 25 INTERIM STRUCTURES. (1) PORTABLE RESTROOMS, AND ASSOCIATED PARKING AREAS. THE INTERIM TRACK OPERATION AREA CONSISTS OF THE ASPHALT TRACK, PADDOCK, (2) PRE-ENGINEERED BUILDINGS, (3) WOODEN RACE, A MAINTENANCE STRUCTURE, SALES STRUCTURE, SHADE STRUCTURE, (4) STORAGE CONTAINERS, PORTABLE RESTROOM, VIEW LIFT, AC LINTS AND 10 GENERATOR. THE INTERIM DRIVING INSTRUCTION AREA CONSISTS OF AN ASPHALT TRACK, SHAD PAD, PADDOCK, 20 WOODEN STRUCTURES, AND A PORTABLE RESTROOM.

**AREAS 4.1 & 4.2**  
 WITHIN THE INTERIM SALES AND CLUB FACILITY THE TEMPORARY SALES, ADMINISTRATION AND CONSTRUCTION TRAILERS ARE EACH 12 FEET WIDE BY 40 FEET LONG WITH A HEIGHT OF APPROXIMATELY 12 FEET. DIMENSIONS ON INTERIM STRUCTURES WILL BE: 10'0" BY 6'0" BY 8'0" OR 12'0" BY 8'0" WITH A 10'3" OF UNLOADING PAD. USES IN THE INTERIM SALES AND CLUB FACILITIES INCLUDES TRACK OPERATIONS, MARKETING / SALES, STORAGE, SERVICE AND TRANSPORT PARKING, FOOD PREPARATION & SERVICE, PARKING, THERMAL CLUB BRANDED MERCHANDISE SALES (SIMILAR TO A GOLF PRO SHOP), VEHICLE MAINTENANCE, VEHICLE STORAGE, AND CAR WASHING.

**AREA 4.3**  
 THERE ARE THREE INTERIM STRUCTURES WITHIN THE TEMPORARY TRACK OPERATION AREA ON THE PADDOCK. THE MAINTENANCE STRUCTURE IS 4' X 10' AND WILL BE USED AS AN AREA TO STORE THE TRACK VEHICLES. THE SALES STRUCTURE IS 8' X 10' AND WILL BE USED BY STAFF TO PROMOTE THE THERMAL CLUB RACING FACILITY TO POTENTIAL MEMBERS. THE SHADE STRUCTURE IS 4' X 10' AND WILL BE USED FOR PARKING THE TRACK VEHICLES OUT OF THE DIRECT SUNLIGHT. TWO STORAGE CONTAINERS IN THIS AREA MEASURE 3' X 6' AND ARE WEATHER 8' X 20' AND THE PORTABLE RESTROOM MEASURES 6' X 12'. THE TWO PRE-ENGINEERED BUILDINGS MEASURE 30' X 10' AND 40' X 10' EACH AND WILL BE USED FOR TRACK OPERATIONS. THE 4' X 10' AND 4' X 12' MEASUREMENTS ARE ADDITIONAL UNITS TO SERVICE TRACK VEHICLES. IN ADDITION TO TRACK OPERATIONS, USES IN THE PADDOCK AND INTERIM TRACK OPERATION AREA INCLUDES SALES, ADMINISTRATION, MARKETING / SALES, THE THERMAL CLUB BRANDED MERCHANDISE SALES (SIMILAR TO A GOLF PRO SHOP), VEHICLE STORAGE, TRUCK STORAGE, VEHICLE LOADING / UNLOADING, VEHICLE MAINTENANCE, OWNER RESTROOM, AND CAR WASHING.

**AREA 4.4**  
 WITHIN THE INTERIM DRIVING INSTRUCTION AREA WILL BE A 80' X 10' INTERIM STRUCTURE, 40' X 10' INTERIM STRUCTURE, PORTABLE RESTROOM, PADDOCK, SHAD PAD, AND ASPHALT TRACK. USES IN THE INTERIM DRIVING INSTRUCTION AREA INCLUDES DRIVING INSTRUCTION, SAFETY, OUTDOORS, SHAD PAD, MARKETING / SALES, THE THERMAL CLUB BRANDED MERCHANDISE SALES (SIMILAR TO A GOLF PRO SHOP), VEHICLE STORAGE, TRUCK STORAGE, VEHICLE LOADING / UNLOADING, VEHICLE MAINTENANCE, OWNER RESTROOM, AND CAR WASHING.

THE ACTIVITIES UNDER THIS WHOR PLOT PLAN WILL BE CONFINED TO THE LOCATIONS SPECIFIED HEREIN AND THE ACTIVITIES WILL BE RESTRICTED TO THOSE DESCRIBED IN THIS PROJECT DESCRIPTION. INTERIM STRUCTURES WILL BE PERMITTED THROUGHOUT THE DEPARTMENT OF BUILDING AND SAFETY PRIOR TO CONSTRUCTION. IN ADDITION, GRADING AND DRAINAGE PLANS WILL BE PREPARED FOR THE INTERIM STRUCTURES. ACTIVITIES IN THE INTERIM FACILITIES WILL BE LIMITED TO PEOPLE PER ACRE BY ALC ZONE C, 100 PEOPLE PER ACRE IN ALC ZONE D, AND 300 PEOPLE PER ACRE IN ALC ZONE D. EVENTS IN ZONE D FOR MORE THAN 200 PEOPLE WILL OBTAIN A TEMPORARY OUTDOOR EVENT PERMIT.

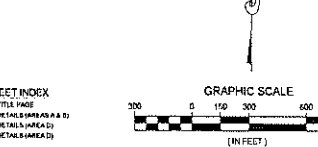
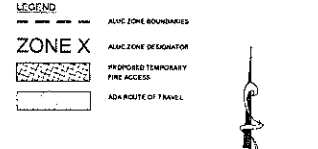
- NOTES**
- NO SUBSURFACE SEPTIC SERVICE INDICATED.
  - LAND IS ANIMAL LOCATIONS POTENTIAL AREA AND IS NOT WITHIN A FAULT HAZARD ZONE.
  - LAND IS NOT SUBJECT TO DEWPOINT INUNDATION OR FLOOD HAZARD.
  - FLOOD ZONE 2 AND 3 ARE AS SHOWN ON FEMA PANEL NO. 58000C0295 EFFECTIVE DATE AUGUST 26, 2006.
  - THERE ARE NO EXISTING WELLS ON THE PROPERTY.
  - PROPOSED ON-SITE OPEN CHANNEL AS INDICATED ON MAP.
  - THE PROJECT WILL COMPLY WITH ALL RESOURCES.
  - TRACK FACILITY LOTS ARE LOTS 216, 218, 217 OF PM 36451 AND LOTS 214-216 AS NOTED IN PERMITS.
  - SHUTTLE SERVICES ARE AVAILABLE ON SITE AT ALL TIMES TO SHUTTLE MEMBERS FROM THE INTERIM SALES AND CLUB FACILITY TO THE INTERIM TRACK OPERATION AREA AND AROUND THE FACILITY AS NEEDED.
  - MAINTENANCE FOR ALL STRUCTURES INCLUDING BUT NOT LIMITED TO ALL TRASH AND WASTE WATER REMOVAL IS PROVIDED BY BURBANK AND ROLF ROOFER.
  - POTABLE WATER IS PROVIDED TO THE FACILITY FROM THE LOCAL WYHOMAN VALLEY A CERTIFIED POTABLE WATER TREAT TO ALL STRUCTURES FOR WASHING HANDS AND DISHES.
  - DRINKING WATER IS PROVIDED AT ALL STRUCTURES THROUGH PURCHASED PURIFIED WATER IN INDIVIDUAL BOTTLES.
  - FOOD SERVICES ARE PROVIDED BY ON-SITE FOOD VENDORS AND CATERERS.
  - TEMPORARY FIRE SUPPRESSION WILL BE USED AT ALL LOCATIONS WHEN PERMANENT WATER HAS BEEN PROVIDED TO THE SITE.
  - A WARNING SIGN READING "WARNING PROXIMITY TO HOT WATERS" BE INSTALLED ON THE AREA OF THE HOLES LINE THAT IS THE ASSIGNED HOLES LINE. THE TRACKS AND DRAINAGE PLANS WILL BE PERMITTED WHERE STANDARDS CONDITIONED UNDER PLOT PLAN 24800 AND DEMONSTRATED PER HOUSE ENFORCEMENT PREPARED BY CONSTRUCTION DEPARTMENT, DATED 02/14/2015.
  - THIS PROJECT WAS FAST TRACK APPROVED UNDER 17A 2011-011.

**USES**

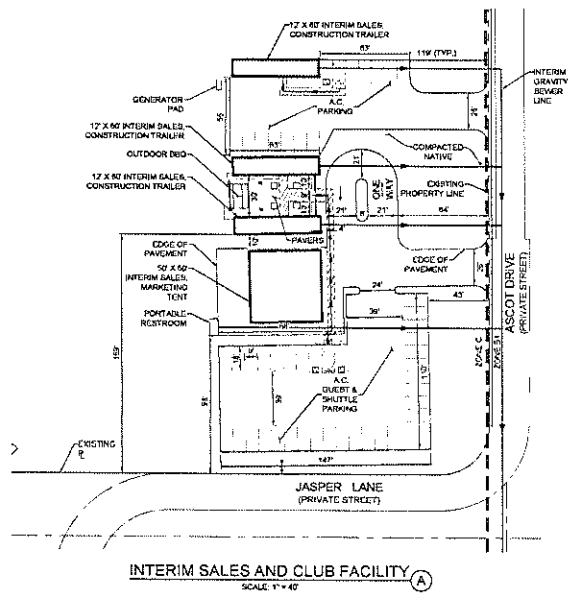
GENERAL	PROCESS
TRACK OPERATIONS	EMTING
MARKETING / SALES	AUTOCRODS
STORAGE	SIDE FINE
VEHICLE AND TRANSPORT PARKING	MARKETING / SALES
FOOD PREPARATION & SERVICE	TRACK OPERATIONS
PARKING	TIME TESTING
MERCHANDISE SALES	MERCHANDISE SALES
VEHICLE MAINTENANCE	VEHICLE STORAGE
CAR WASHING	TRUCK STORAGE
	VEHICLE LOADING / UNLOADING
	VEHICLE MAINTENANCE
	CAR WASHING

PROJECT DATA	
APPLICANT	THERMAL OPERATIONS COMPANY, LLC 1830 N. HENRI STREET, SUITE 100 THERMAL, CA 92580
OWNER	JTM LAND COMPANY, LLC 280 W. 180TH STREET, SUITE 100 THERMAL, CA 92580 (951) 488-4772
ENGINEER	KEE CONSULTANTS INC. 12323 MILL CREEK DRIVE, SUITE 208 LA HABRA, CA 91708 (951) 493-9111
ADJACENT PARCEL NUMBERS	789-100-02 (TO THE WEST), 789-100-01 (TO THE EAST), 789-100-04 (TO THE SOUTH), 789-100-05 (TO THE NORTH)
DATE OF PREPARATION	APRIL 2015
THOMAS BROTHERS MAP (2008 ED.)	4, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 54, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 97, 98, 99, 100
EXISTING ZONING	SPRINKLE MAP 300, THE EDN. RANCHO (SPR), E, 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 54, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 97, 98, 99, 100
PROPOSED ZONING	SPRINKLE MAP 300, THE EDN. RANCHO (SPR), E, 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 54, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 97, 98, 99, 100
UTILITY PROVIDERS	WATER: CALIFORNIA VALLEY WATER DISTRICT SEWER: THE GAS COMPANY ELECTRICITY: CALIFORNIA VALLEY DISTRICT TELEPHONE: CALIFORNIA VALLEY DISTRICT CABLE TELEVISION: TIME WARNER CABLE
COUNTY SERVICE AREA	THERMAL #25
SCHOOL DISTRICT	COMPELLA VALLEY UNIFIED SCHOOL DISTRICT
DATE OF PERMIT	5 YEARS
STORAGE	214 X 30' ON 30' TALL PEDESTAL

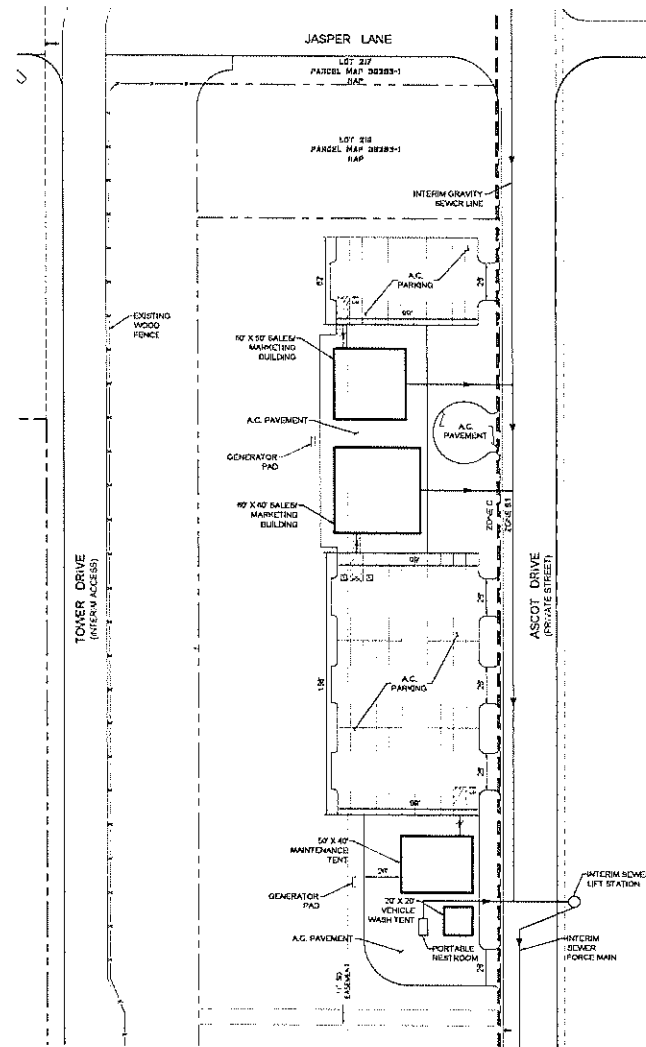
BUILDING LEGEND AND DATA			
BUILDING NAME	SIZE (SQ. FT.)	CONSTRUCTION TYPE	OCCUPANCY
INTERIM SALES	4,732	VS	B
TRUCK FACILITY	1,500	VS	B
INTERIM MARKET	8,232	VS	B
INTERIM TRACK OPERATION AREA	11,892	VS	B
INTERIM DRIVING INSTRUCTION AREA	8,032	VS	B



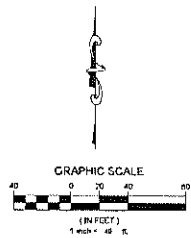
REVISIONS DATE NO. DRAWN BY: TP CHECKED BY: MC DATE: 02/10/2014 SCALE: 1" = 300' FILE NO. 0590-100	DESIGNED BY: [Signature] DRAWN BY: TP CHECKED BY: MC DATE: 02/10/2014	STAMP BASIS OF BEARINGS BENCHMARK APPROVED BY: [Signature] IN THE UNINCORPORATED TERRITORY OF THERMAL, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA	MINOR PLOT PLAN NO. 25436 INTERIM SALES & OPERATIONS THE THERMAL CLUB BEING A PORTION OF THE SE 1/4 OF SECTION 33 T.6S, R.8E, S.B.B.M.	DRAWING NAME: MPP 25436 01 TL PROJECT NO. 0590-100 SHEET 1 OF 4




**INTERIM SALES AND CLUB FACILITY**  
SCALE: 1" = 40'

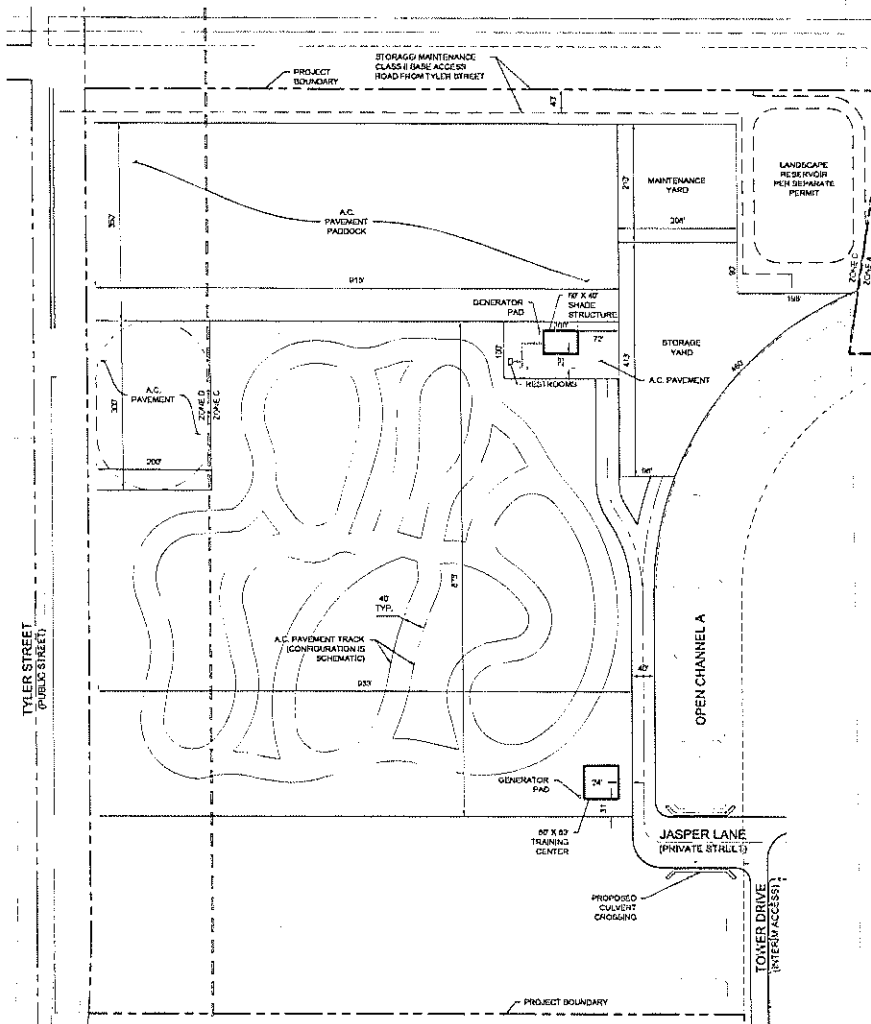


**INTERIM MARKETING FACILITIES**  
SCALE: 1" = 40'

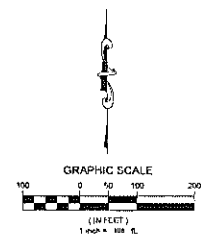


 R.C. Consultants, Inc.		25337 Hill Drive Dr Suite 205 Laguna Hills, CA 92653 Phone: 949-453-0111 Fax: 949-453-0411	<b>STAMP</b> _____ _____ _____	<b>BASIS OF BEARINGS</b> BEARINGS ARE BASED UPON THE NORTHWEST CORNER OF THE NORTHWEST QUARTER OF SECTION 33, TOWNSHIP 8 SOUTH, RANGE 8 WEST, S.B.M. 45 BEING INFORMATION AS SHOWN BY RECORDS OF SURVEY ON FILE IN BOOK 101 41 PAGE 31 THROUGH 41 TUNICUM, RECORDS OF HAVESIDE COUNTY, CALIFORNIA.	<b>BENCH MARK</b> CALIFORNIA HIGHWAY 100-14-11 1 2 1/2" X 2" 5000.00 IN. W. END OF 14" X 14" S.W. CORNER CORNER OF INTERSECTION OF 14" X 14" S.W. CORNER CALIFORNIA HIGHWAY 100, TRANSFORMATION 1988 11-14-88 S. 1/4 1/4 BELOW GROUND AND 3' S.E. OF 14" X 14" BENCHMARK ELEVATION = 375.33 USNGMS DATUM +1987 1972 ADJUSTMENT	<b>APPROVED BY:</b> _____ _____	IN THE UNINCORPORATED TERRITORY OF THERMAL COUNTY, AVENUE, STATE OF CALIFORNIA <b>MINOR PLOT PLAN NO. 25436</b> <b>INTERIM SALES &amp; OPERATIONS</b> <b>THE THERMAL CLUB</b> BEING A PORTION OF THE SE 1/4 OF SECTION 33 T 8S. R 8E. S.B.R.M.	<b>DRAWING NAME:</b> MPP 25436 02 DT <b>PROJECT No.</b> 0580-100 <b>SHEET 2 OF 4</b>
<b>REVISIONS</b> _____ _____ _____	<b>DATE</b> _____ _____	<b>DESIGNED BY:</b> RL <b>DRAWN BY:</b> IT	<b>CHECKED BY:</b> RC <b>DATE:</b> 02/10/2014	<b>SCALE:</b> AS SHOWN	<b>FILE NO.</b> 0590-100			





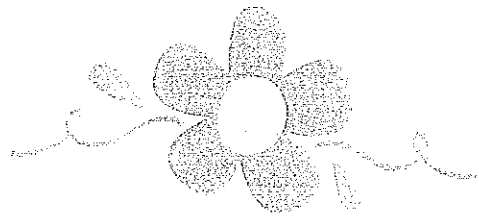
INTERIM DRIVING INSTRUCTION AREA D  
SCALE: 1" = 100'



		21537 Mid Creek Dr. Suite 205 Laguna Hills, Ca. 92653 Phone: 949.453.0111 Fax: 949.453.0411	<b>STAMP</b> BASIS OF BEARINGS BEARINGS ARE BASED UPON THE METRIC LINE OF THE NORTHERN ONE-QUARTER OF SECTION 33, TOWNSHIP 3 SOUTH, RANGE 8 EAST, S.E.W. 43, BEING INDICATED AS SHOWN BY RECORD OF SURVEY FOR FILE IN BOOK 18, 43 PAGES 18 THROUGH 41 WITH RECORDS OF "MONSIEUR COUNTY, CALIFORNIA.	<b>BENCH MARK</b> CAL TRANS DIV. 100 TRINITY 4.1, 20' (2") BENCH MARK IN SECTION 4, T. 3 N., R. 8 E. CALIFORNIA DEPT. OF TRANSPORTATION 1968-1974 S. L. 0.3 BELOW GROUND AND 3' S.E. OF PIP 450-3450 ELEVATION = 315.33 USER DATE = 2007 1972 ADJUSTMENT	<b>APPROVED BY:</b> _____ _____ _____	IN THE UNINCORPORATED TERRITORY OF THERMAL COUNTY OF NIVERSIDE STATE OF CALIFORNIA <b>MINOR PLOT PLAN NO. 25436</b> INTERIM SALES & OPERATIONS THE THERMAL CLUB BEING A PORTION OF THE SE 1/4 OF SECTION 33 T. 05, R. 08, S. R. 0 M	<b>DRAWING NAME:</b> MPP 25436 04 DT <b>PROJECT No.</b> 0590-100 <b>SHEET 4 OF 4</b>
<b>REVISIONS</b> _____ _____ _____	<b>DATE NO</b> DESIGNED BY: HL DRAWN BY: T	CHECKED BY: HC DATE: 02/10/2014 SCALE: AS SHOWN	FILE No. 0590-100				



**PAGE BREAK**





# AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

March 5, 2014

**CHAIR**  
Simon Housman  
Rancho Mirage

**VICE CHAIRMAN**  
Rod Ballance  
Riverside

**COMMISSIONERS**

Arthur Butler  
Riverside

Glen Holmes  
Hemet

John Lyon  
Riverside

Greg Pettis  
Cathedral City

Richard Stewart  
Moreno Valley

**STAFF**

Director  
Ed Cooper

John Guerin  
Russell Brady  
Barbara Santos

County Administrative Center  
4080 Lennon St., 14th Floor.  
Riverside, CA 92501  
(951) 955-5132

[www.rcaluc.org](http://www.rcaluc.org)

Mr. Glenn Mlaker, Assistant Planner  
City of Palm Springs Planning Services Department  
3200 E Tahquitz Canyon Way  
Palm Springs, CA 92262

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW**

File No.: ZAP1019PS14  
Related File No.: 5.0793 CUP, 6.535 VAR  
APN: 680-120-032

Dear Mr. Mlaker:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC), staff reviewed the above-referenced proposal for the establishment of an unmanned telecommunications facility consisting of antennas on a 43-foot high monopole, with associated equipment shelter, on a 900 square foot lease area within a 2.65-acre parcel owned by the City of Palm Springs located southerly of Mesquite Avenue, westerly of Mountain View Drive, and easterly of California Avenue, in the City of Palm Springs.

The site is located within Airport Compatibility Zone E of the Palm Springs International Airport Influence Area (AIA). Given the site's proximity to the runway (approximately 4,500 feet), the applicant filed Form 7460-1 with the Federal Aviation Administration Obstruction Evaluation Service and obtained a "Determination of No Hazard to Air Navigation." (Therefore, ALUC did not charge an additional fee for review of the variance in this case.)

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2005 Palm Springs International Airport Land Use Compatibility Plan, subject to the following conditions:

**CONDITIONS:**

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky.
2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.

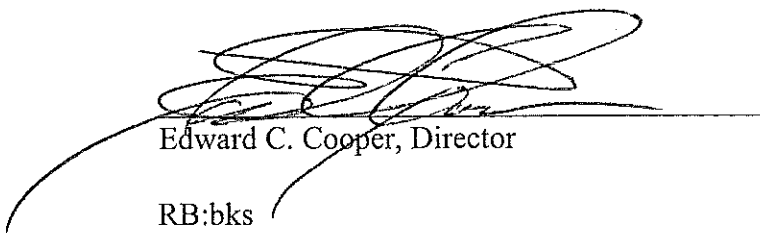
- (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area, including but not limited to, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, artificial marshes, incinerators, and fly ash disposal.
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached notice shall be provided to all potential purchasers of the property and tenants of any habitable buildings thereon.
  4. The Federal Aviation Administration has conducted an aeronautical study of the proposed structure (Aeronautical Study No. 2013-AWP-8007-OE) and has determined that neither marking nor lighting of the structure is necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 K Change 2 and shall be maintained in accordance therewith for the life of the project.
  5. The maximum height of the proposed structure shall not exceed 43 feet above ground level, and the maximum elevation of the proposed structure, including all towers and antennas, shall not exceed 420 feet above mean sea level.
  6. The specific coordinates, height, top point elevation of the proposed structure, frequencies, and power shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in building height or elevation shall not require further review by the Airport Land Use Commission.
  7. Temporary construction equipment used during actual construction of the structure shall not exceed the height of the structure, unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.
  8. Within five (5) days after construction of the structure reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned.

9. The telecommunications facility shall be designed in such a manner as to ensure that spurious emissions signal levels from the proposed transmitters will be less than -104 dBm in the 108-137 and 225-400 MHZ frequency bands at a distance of 9,000 feet from the transmitter site, in accordance with the requirements of the Federal Aviation Administration Obstruction Evaluation Service letter dated January 24, 2014, a copy of which is attached hereto and incorporated herein by reference.

If you have any questions, please contact Russell Brady, Contract Planner, at (951) 955-0549, or John Guerin, Principal Planner, at (951) 955-0982.

Sincerely,

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Edward C. Cooper, Director

RB:bks

Attachments: Notice of Airport in Vicinity  
FAA Aeronautical Study No. 2013-AWP-8007-OE

cc: Thomas Nolan, Manager, Palm Springs International Airport  
Brett Smirl, Spectrum Services  
Spectrum Services, Las Vegas (payee)  
ALUC Staff

Y:\AIRPORT CASE FILES\Palm Springs\ZAP1019PS14\ZAP1019PS14LTR.doc



# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
2601 Meacham Boulevard  
Fort Worth, TX 76193

Aeronautical Study No.  
2013-AWP-8007-OE

Issued Date: 01/24/2014

Jim O'Dowd  
Verizon Wireless  
180 Washington Valley Rd  
Bedminster, NJ 07921

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Monopole Hollyhock  
Location: Palm Springs, CA  
Latitude: 33-48-34.88N NAD 83  
Longitude: 116-30-29.79W  
Heights: 377 feet site elevation (SE)  
43 feet above ground level (AGL)  
420 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part I)  
 Within 5 days after the construction reaches its greatest height (7460-2, Part II)

**See attachment for additional condition(s) or information.**

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed and maintained in accordance with FAA Advisory circular 70/7460-1 K Change 2.

This determination expires on 07/24/2015 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates , heights, frequency(ies) and power . Any changes in coordinates , heights, and frequencies or use of greater power will void this determination. Any future construction or alteration , including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

A copy of this determination will be forwarded to the Federal Communications Commission (FCC) because the structure is subject to their licensing authority.

If we can be of further assistance, please contact our office at (817) 321-7760. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2013-AWP-8007-OE.

**Signature Control No: 204611566-206185185**  
Joan Tengowski  
Technician

( DNE )

Attachment(s)  
Additional Information  
Frequency Data  
Map(s)

cc: FCC

**Additional information for ASN 2013-AWP-8007-OE**

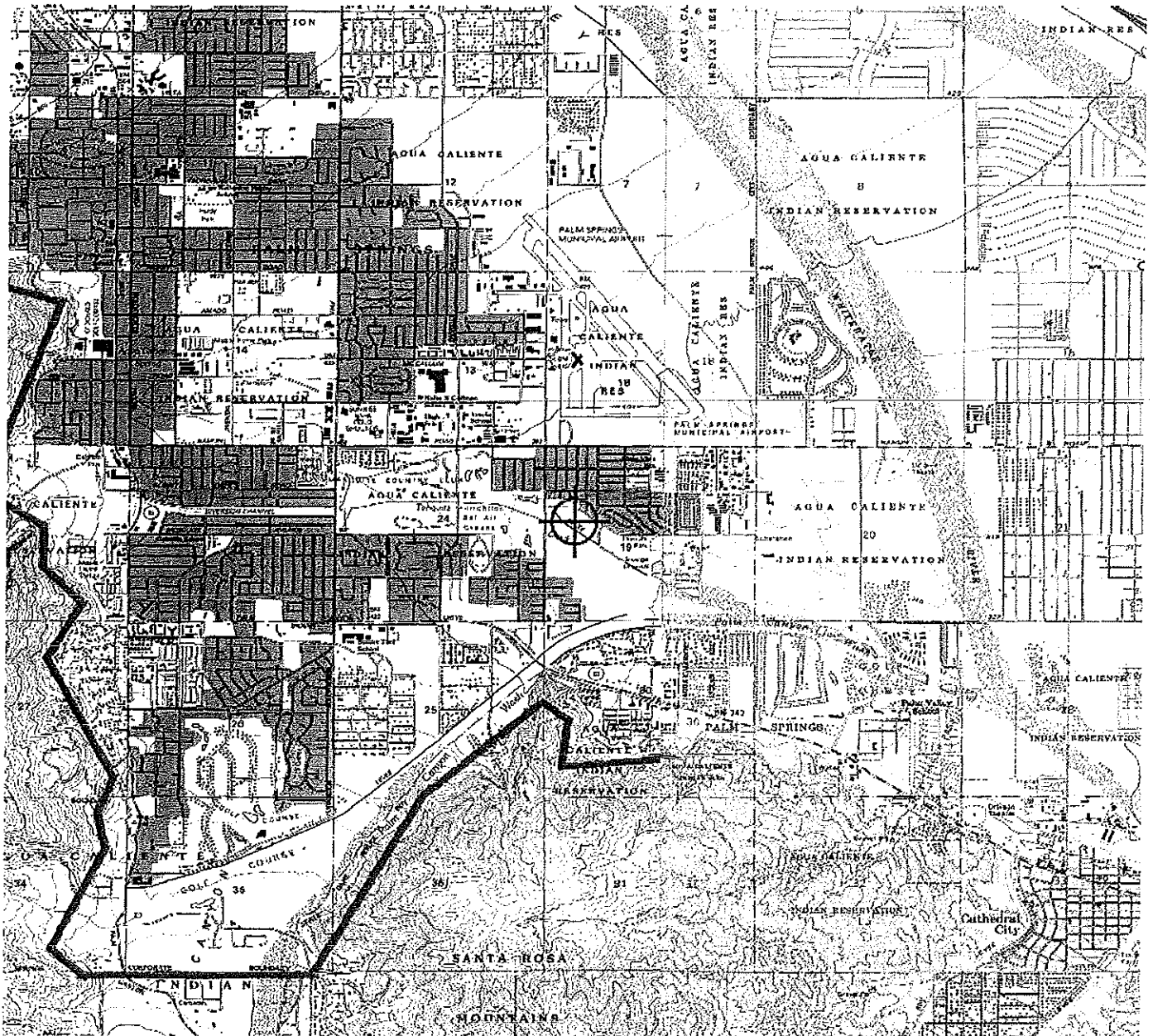
At a distance of 9000 feet from transmitter site spurious emissions signal levels from proposed transmitters must be less than -104 dBm in the 108-137, 225-400 MHz frequency bands.



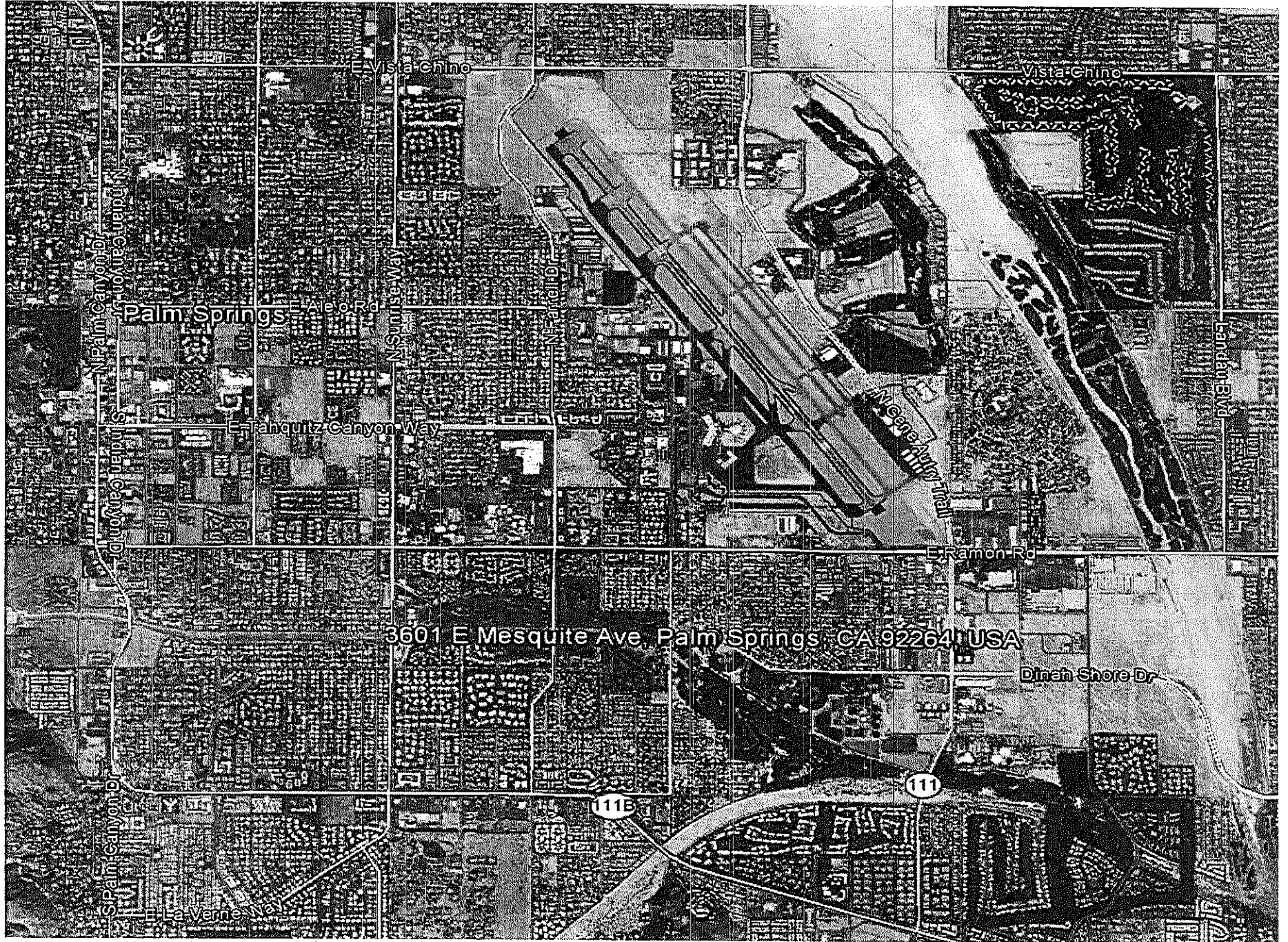
Frequency Data for ASN 2013-AWP-8007-OE

LOW FREQUENCY	HIGH FREQUENCY	FREQUENCY UNIT	ERP	ERP UNIT
698	806	MHz	1000	W
806	824	MHz	500	W
824	849	MHz	500	W
851	866	MHz	500	W
869	894	MHz	500	W
896	901	MHz	500	W
901	902	MHz	7	W
930	931	MHz	3500	W
931	932	MHz	3500	W
932	932.5	MHz	17	dBW
935	940	MHz	1000	W
940	941	MHz	3500	W
1850	1910	MHz	1640	W
1930	1990	MHz	1640	W
2305	2310	MHz	2000	W
2345	2360	MHz	2000	W

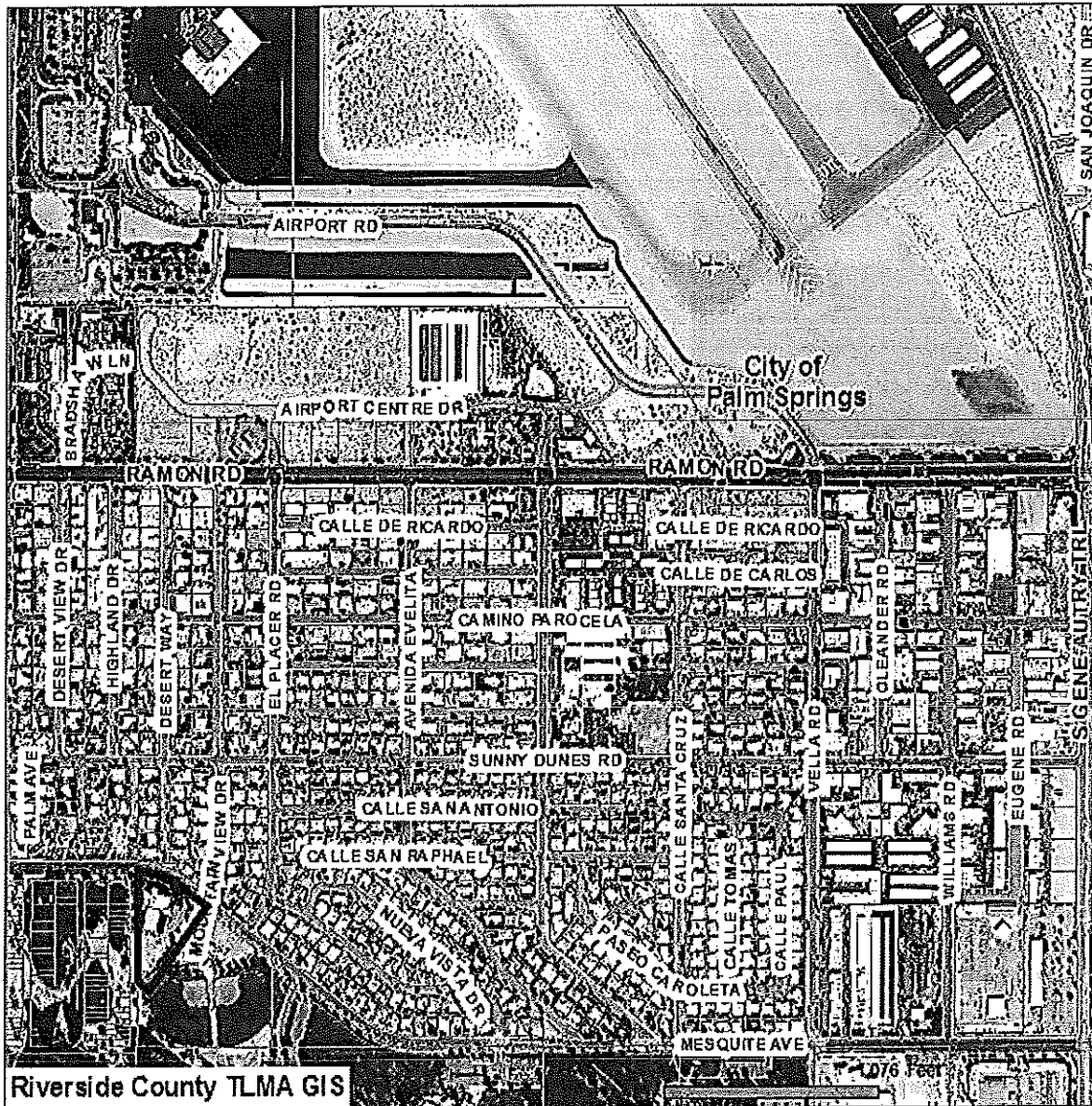
TOPO Map for ASN 2013-AWP-8007-OE



Project In Relationship to Airport Hollyhock)



RIVERSIDE COUNTY GIS



Selected parcel(s):  
680-120-032

**\*IMPORTANT\***

Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON...Fri Feb 28 12:21:58 PST 2014  
Version 131127



RIVERSIDE COUNTY GIS



Selected parcel(s):  
680-120-032

**\*IMPORTANT\***

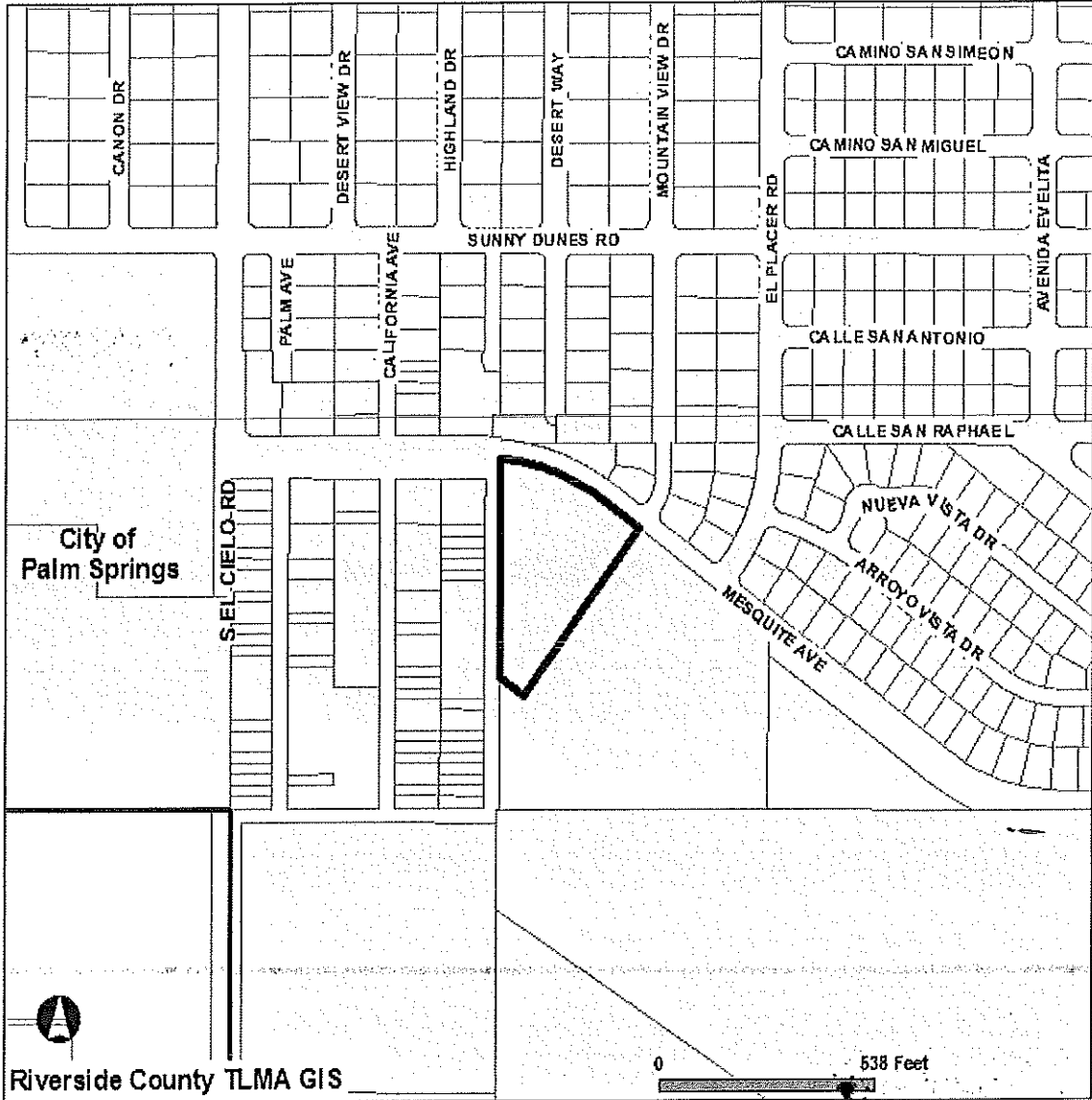
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REPORT PRINTED ON...Fri Feb 28 12:19:45 PST 2014

Version 131127



RIVERSIDE COUNTY GIS



Selected parcel(s):  
680-120-032

AIRPORTS

- SELECTED PARCEL
- AIRPORT INFLUENCE AREAS
- INTERSTATES
- HIGHWAYS
- PARCELS
- COMPATIBILITY ZONE C
- COMPATIBILITY ZONE E

**\*IMPORTANT\***

Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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Version 131127



15505 SAND CANYON AVENUE  
BUILDING D, 1ST FLOOR  
IRVINE, CALIFORNIA 92618

# HOLLYHOCK

3601 MESQUITE AVENUE  
PALM SPRINGS, CALIFORNIA 92264



PROJECT INFORMATION  
**HOLLYHOCK**  
3601 MESQUITE AVENUE  
PALM SPRINGS, CALIFORNIA 92264

CURRENT ISSUE DATE:  
12/03/13

ISSUED FOR:  
**ZONING**

REV.: DATE: DESCRIPTION: BY:

REV.	DATE	DESCRIPTION	BY
12/03/13		100K ZONING	T.R.
11/26/13		90K ZONING	R.C.

PLANS PREPARED BY:  
**SPECTRUM**  
SPECTRUM SERVICES, INC.  
8390 MAPLE PLACE, SUITE 110  
RANCHO CUCAMONGA, CA 91730  
PH: (866) 515-3358  
FAX (866) 515-3359

LICENSURE:

TITLE	SIGNATURE	DATE
RF ENGINEER		
REAL ESTATE		
PROPERTY OWNER		
ZONING APPROVAL		
CONSTRUCTION DIRECTOR		
ADDITIONAL APPROVAL		

SHEET TITLE:  
**TITLE SHEET**

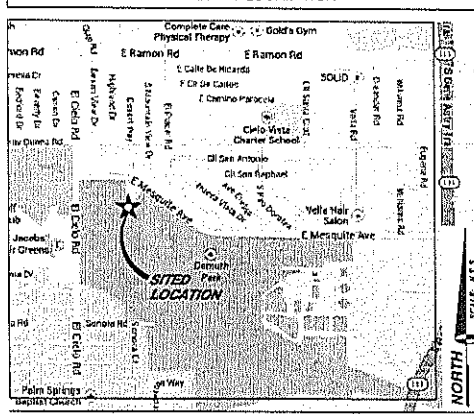
SHEET NUMBER: REVISION:

**T1** **1**  
LAX-299

THE PROPOSED PROJECT INCLUDES:

- INSTALLATION OF A 900 SQ. FT. VERIZON WIRELESS FACILITY
- INSTALLATION OF (12) VERIZON WIRELESS PANEL ANTENNAS AT A 40' CENTERLINE MOUNTED ON A NEW 42' MONOPOLE
- INSTALLATION OF A VERIZON WIRELESS PARABOLIC ANTENNA
- INSTALLATION OF VERIZON WIRELESS 11'-6" X 16'-10.5" EQUIPMENT SHELTER
- INSTALLATION OF (2) VERIZON WIRELESS GPS ANTENNAS
- INSTALLATION OF A VERIZON WIRELESS 8" WROUGHT IRON FENCE
- INSTALLATION OF A VERIZON WIRELESS 4" WIDE WROUGHT IRON GATE
- INSTALLATION OF (6) VERIZON WIRELESS REMOTE RADIO UNITS (RRUs) (2 PER SECTOR)
- INSTALLATION OF (1) TOWER MOUNTED JUNCTION BOX
- INSTALLATION OF A VERIZON WIRELESS 30KW GENERATOR WITH 211 GALLON DIESEL TANK ON A 5' X 9' CONCRETE PAD
- COAXIAL/ HYBRIFLEX CABLE RUNS FROM RADIOS TO ANTENNAS
- NEW TELEPHONE CONDUIT RUN TO CABINETS
- NEW 200A DEDICATED ELECTRICAL SERVICE TO METER

**PROJECT DESCRIPTION**



VICINITY MAP

**APPLICANT/LESSEE**  
VERIZON WIRELESS  
15505 SAND CANYON AVENUE  
BUILDING D, 1ST FLOOR  
IRVINE, CALIFORNIA 92618  
(949)205-7000

**PROPERTY INFORMATION**  
OWNER: CITY OF PALM SPRINGS  
3200 EAST TAHQUITZ CANYON WAY  
PALM SPRINGS, CALIFORNIA 92263

AREA OF CONSTRUCTION: 900± SQ. FT.  
OCCUPANCY TYPE: S-2  
CONSTRUCTION TYPE: V-B  
CURRENT ZONING: OPEN LAND ZONE (O)/WATERCOURSE (100-500YR FLOODPLAIN FEMA '08)

JURISDICTION: CITY OF PALM SPRINGS  
APN: 680-120-032  
HANDICAP REQUIREMENTS: FACILITY IS UNMANNED AND NOT FOR HUMAN HABITATION. HANDICAPPED ACCESS NOT REQUIRED.

**PROJECT SUMMARY**

ALL WORK AND MATERIALS SHALL BE PERFORMED AND INSTALLED IN ACCORDANCE WITH THE CURRENT EDITIONS OF THE FOLLOWING CODES AS ADOPTED BY THE LOCAL GOVERNING AUTHORITIES. NOTHING IN THESE PLANS IS TO BE CONSIDERED TO PERMIT WORK NOT CONFORMING TO THESE CODES.

- 2010 CALIFORNIA ADMINISTRATIVE CODE
- 2010 CALIFORNIA BUILDING STANDARDS CODE
- 2010 CALIFORNIA ELECTRICAL CODE
- 2010 CALIFORNIA MECHANICAL CODE
- 2010 CALIFORNIA PLUMBING CODE
- 2010 CALIFORNIA ENERGY CODE
- 2010 CALIFORNIA FIRE CODE

**CODE COMPLIANCE**

**SITE ACQUISITION / PLANNING**  
SPECTRUM SERVICES, INC.  
8390 MAPLE PL, SUITE 110  
RANCHO CUCAMONGA, CALIFORNIA 91730  
MIKE HAYES  
PHONE: (866) 515-3358  
FAX: (866) 515-3359

**STRUCTURAL ENGINEER:**  
T.B.O.

**ELECTRICAL ENGINEER:**  
DES CONSULTING ENGINEERING SERVICES LLC  
6811 W. CHARLESTON BOULEVARD, SUITE 2539  
LAS VEGAS, NEVADA 89117  
DEREK G. STEFUREAC  
PHONE: (702) 885-1552

**SURVEYOR:**  
DIAMONDBACK LAND SURVEYING  
1301 SOUTH SHARROUN ROAD, SUITE A1  
LAS VEGAS, NEVADA 89145  
TRICHT J. KEENAN  
PHONE: (702) 823-3257

**PROJECT TEAM**

SHEET	DESCRIPTION	REV.
T1	TITLE SHEET	▲
A1	SITE PLAN	▲
A2	SITE DETAIL, ANTENNA & CABLE SCHEDULE AND ANTENNA LAYOUT	▲
A3	EAST AND SOUTH ELEVATIONS	▲
A4	WEST AND NORTH ELEVATIONS	▲

**PROJECT TEAM**

SHEET INDEX	ZONING

TITLE	SIGNATURE	DATE
RF ENGINEER		
REAL ESTATE		
PROPERTY OWNER		
ZONING APPROVAL		
CONSTRUCTION DIRECTOR		
ADDITIONAL APPROVAL		

**APPROVAL LIST**

**NOTES & TITLE REPORT EXCEPTIONS**

ONLY THOSE EXCEPTION(S) LISTED IN TITLE REPORT ORDER NO. 0019706-990-002, DATED OCTOBER 1, 2013, PREPARED BY PCOR TITLE COMPANY OF CALIFORNIA, WHICH ARE NOT SOLELY FINANCIAL IN NATURE AND WHICH REFERENCE A DOCUMENT CONTAINING A SUFFICIENT LEGAL DESCRIPTION OF AREAS AFFECTED BY SAID DOCUMENT WERE CONSIDERED FOR THIS SURVEY AND RE-LISTED BELOW. ITEM NUMBERS CORRESPOND TO ITEMS NUMBERS IN SAID TITLE REPORT.

⑤ EASEMENT(S) FOR THE PURPOSE(S) SHOWN BELOW AND RIGHTS INCIDENTAL THEREOF, AS GRANTED IN A DOCUMENT: GRANTED TO: CITY OF PALM SPRINGS  
PURPOSE: STREET AND PUBLIC UTILITIES  
RECORDING DATE: JULY 11, 1978  
RECORDING NO: AS INSTRUMENT NO. 142584 OF OFFICIAL RECORDS

AFFECTS: SAID LAND AND RE-RECORDING DATE: AUGUST 29, 1978 AND RE-RECORDING NO: AS INSTRUMENT NO. 182811 OF OFFICIAL RECORDS  
AFFECTS A PORTION OF THE SUBJECT PROPERTY

⑥ COVENANTS, CONDITIONS AND RESTRICTIONS BUT OMITTING ANY COVENANTS OR RESTRICTIONS, IF ANY, INCLUDING BUT NOT LIMITED TO THOSE BASED UPON RACE, COLOR, RELIGION, SEX, SEXUAL ORIENTATION, FAMILIAL STATUS, MARITAL STATUS, DISABILITY, HANDICAP, NATIONAL ORIGIN, ANCESTRY, SOURCE OF INCOME, GENDER, GENDER IDENTITY, GENDER EXPRESSION, MEDICAL CONDITION OR GENETIC INFORMATION, AS SET FORTH IN APPLICABLE STATE OR FEDERAL LAWS, EXCEPT TO THE EXTENT THAT SAID COVENANT OR RESTRICTION IS PERMITTED BY APPLICABLE LAW, AS SET FORTH IN THE DOCUMENT  
RECORDING DATE: JULY 11, 1978  
RECORDING NO: AS INSTRUMENT NO. 142584 OF OFFICIAL RECORDS

SAID COVENANTS, CONDITIONS AND RESTRICTIONS PROVIDE THAT A VIOLATION THEREOF SHALL NOT DEFEAT THE LICH OF ANY MORTGAGE OR DEED OF TRUST MADE IN GOOD FAITH AND FOR VALUE.  
AND RE-RECORDING DATE: AUGUST 29, 1978 AND RE-RECORDING NO: AS INSTRUMENT NO. 182811 OF OFFICIAL RECORDS  
AFFECTS THE ENTIRE SUBJECT PROPERTY

⑦ EASEMENT(S) FOR THE PURPOSE(S) SHOWN BELOW AND RIGHTS INCIDENTAL THEREOF, AS GRANTED IN A DOCUMENT: GRANTED TO: SOUTHERN CALIFORNIA EDISON COMPANY, A CORPORATION  
PURPOSE: MAINTAIN UNDERGROUND ELECTRICAL SUPPLY SYSTEMS AND COMMUNICATION SYSTEMS  
RECORDING DATE: NOVEMBER 21 1980  
RECORDING NO: AS INSTRUMENT NO. 219236 OF OFFICIAL RECORDS

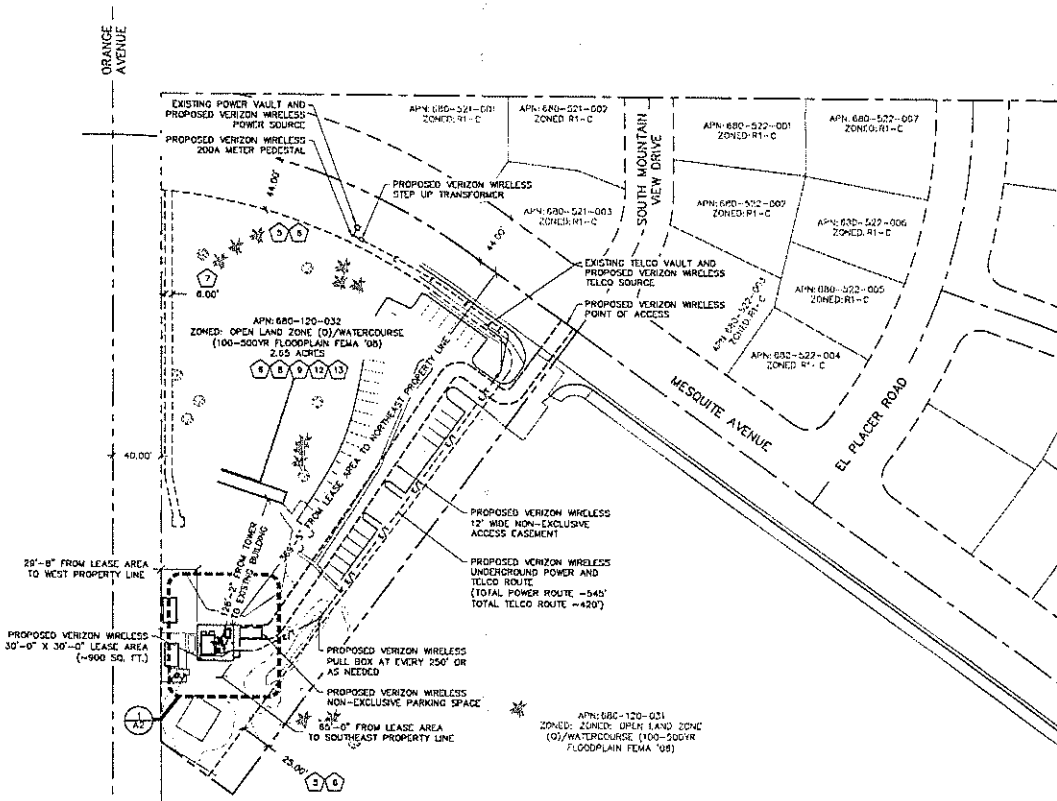
AFFECTS: AS DESCRIBED THEREIN REFERENCE IS HEREBY MADE TO SAID DOCUMENT FOR FULL PARTICULARS.  
AFFECTS A PORTION OF THE SUBJECT PROPERTY

⑧ AN UNRECORDED LEASE WITH CERTAIN TERMS, COVENANTS, CONDITIONS AND PROVISIONS SET FORTH THEREIN AS DISCLOSED BY THE DOCUMENT ENTITLED: MEMORANDUM OF AGREEMENT LESSOR: PALM SPRINGS YOUTH CENTER, A NON-PROFIT CORPORATION LESSEE: NEXTEL OF CALIFORNIA - INC., A DELAWARE CORPORATION D/J/A NEXTEL COMMUNICATIONS  
RECORDING DATE: AUGUST 27, 2001  
RECORDING NO: AS INSTRUMENT NO. 2001-412923 OF OFFICIAL RECORDS  
THE PRESENT OWNERSHIP OF THE LEASEHOLD CREATED BY SAID LEASE AND OTHER MATTERS AFFECTING THE INTEREST OF THE LESSEE ARE NOT SHOWN HEREIN.  
AFFECTS THE ENTIRE SUBJECT PROPERTY

⑨ MATTERS CONTAINED IN THAT CERTAIN DOCUMENT ENTITLED: AGREEMENT RELATING TO REAL PROPERTY RIGHT OF FIRST REFUSAL. CITY OF PALM SPRINGS DATED: JULY 23, 2003 EXECUTED BY: PALM SPRINGS YOUTH CENTER, FAMILY YMCA OF THE DESERT AND THE CITY OF PALM SPRINGS RECORDING DATE: AUGUST 21, 2003 RECORDING NO: AS INSTRUMENT NO. 2003-641887 OF OFFICIAL RECORDS REFERENCE IS HEREBY MADE TO SAID DOCUMENT FOR FULL PARTICULARS.  
AFFECTS THE ENTIRE SUBJECT PROPERTY

⑩ THE EFFECT OF A MEMORANDUM OF AGREEMENT FROM: TORRECO ASSETS LLC, A DELAWARE LIMITED LIABILITY COMPANY TO: ROYAL STREET COMMUNICATIONS CALIFORNIA, LLC A DELAWARE LIMITED LIABILITY COMPANY DATED: FEBRUARY 26, 2010 RECORDING DATE: APRIL 5, 2010 RECORDING NO: AS INSTRUMENT NO. 2010-0154132 OF OFFICIAL RECORDS  
AFFECTS THE ENTIRE SUBJECT PROPERTY

⑪ THE EFFECT OF A MEMORANDUM OF AGREEMENT FROM: TORRECO ASSETS LLC, A DELAWARE LIMITED LIABILITY COMPANY TO: ROYAL STREET COMMUNICATIONS CALIFORNIA, LLC A DELAWARE LIMITED LIABILITY COMPANY DATED: FEBRUARY 26, 2010 RECORDING DATE: DECEMBER 22, 2010 RECORDING NO: AS INSTRUMENT NO. 2010-0614395 OF OFFICIAL RECORDS  
AFFECTS THE ENTIRE SUBJECT PROPERTY



**LEGEND**

---	LEASE AREA
---	CENTERLINE
---	CASSETT
---	RIGHT-OF-WAY
---	SECTION LINE
---	PROPERTY LINE
---	OVERHEAD POWER
---	EXISTING CHAINLINK FENCE
---	PROPOSED CHAINLINK FENCE
---	PROPOSED WROUGHT IRON FENCE
---	EXISTING 1' CONTOUR
---	EXISTING 5' CONTOUR
---	EXISTING BLOCK WALL
---	PROPOSED BLOCK WALL
---	FIRE HYDRANT
---	PARKING LOT AREA LIGHT
---	O.R.
---	POWER POLE

**verizon wireless**  
15505 SAND CANYON AVENUE  
BUILDING D, 1ST FLOOR  
IRVINE, CALIFORNIA 92618

**HOLLYHOCK**  
3601 MESQUITE AVENUE  
PALM SPRINGS, CALIFORNIA 92264

CURRENT ISSUE DATE:  
**12/03/13**

ISSUED FOR:  
**ZONING**

REV: DATE DESCRIPTION BY:


PLANS PREPARED BY:  
**SPECTRUM**  
SPECTRUM SERVICES, INC.  
8300 MAPLE PLACE, SUITE 110  
RANCHO CUCAMONGA, CA 91730  
PH. (866) 515-3358  
FAX (866) 515-3359

LICENSURE:

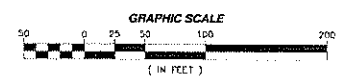
SHEET TITLE:

**SITE PLAN**

SHEET NUMBER: REVISION:

**A1** **1**  
LAX-299

SITE PLAN



11" x 17" SCALE (4" x 36" SCALE)  
1" = 100' 1" = 30'



PROJECT INFORMATION:  
**HOLLYHOCK**  
 3601 MESQUITE AVENUE  
 PALM SPRINGS, CALIFORNIA 92264

CURRENT ISSUE DATE:  
 12/03/13

ISSUED FOR:  
**ZONING**

REV.: DATE: DESCRIPTION: BY:

REV.	DATE	DESCRIPTION	BY
12/03/13		100% ZONING	T.R.
11/26/13		90% ZONING	R.C.

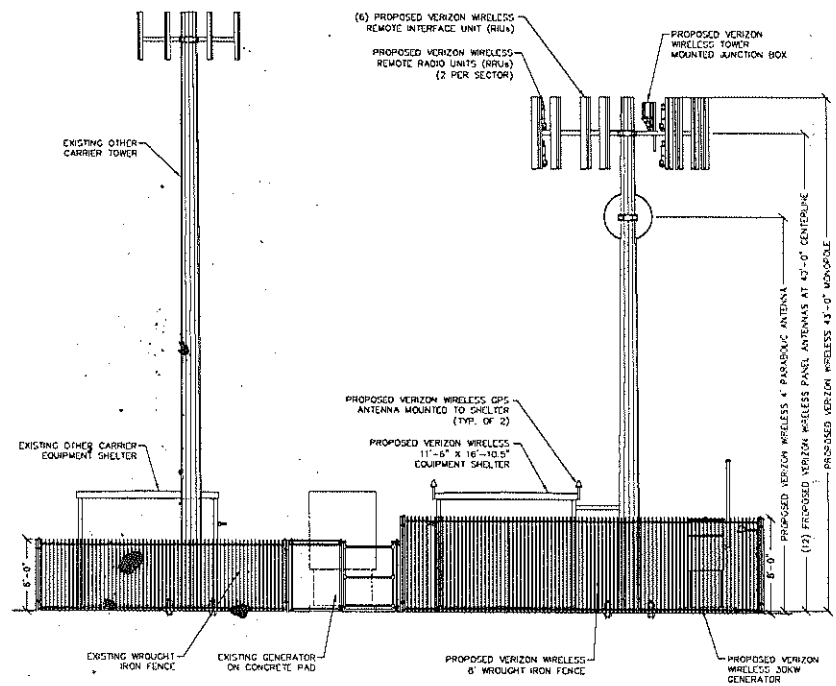
PLANS PREPARED BY:  
**SPECTRUM**  
 SPECTRUM SERVICES, INC.  
 8390 MAPLE PLACE, SUITE 110  
 RANCHO CUCAMONGA, CA 91730  
 PH. (866) 515-3358  
 FAX (866) 515-3359

LICENSURE:

SHEET TITLE:  
**EAST AND SOUTH ELEVATIONS**

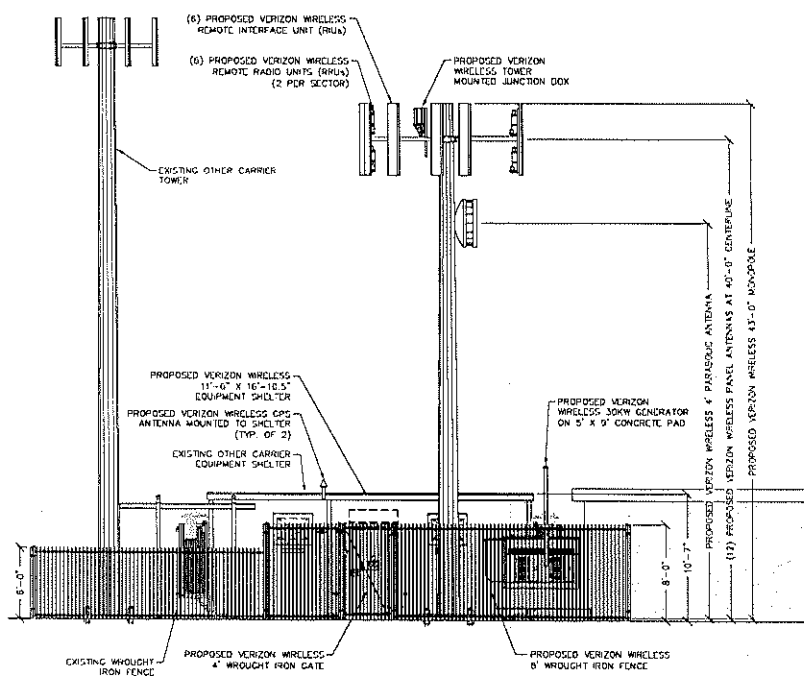
SHEET NUMBER: REVISION:

**A3** 1  
 LAX-299

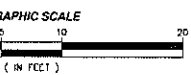


**SOUTH ELEVATION**

11" x 17" SCALE 24" x 36" SCALE  
 1" = 10' 1" = 5'

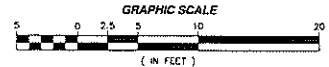
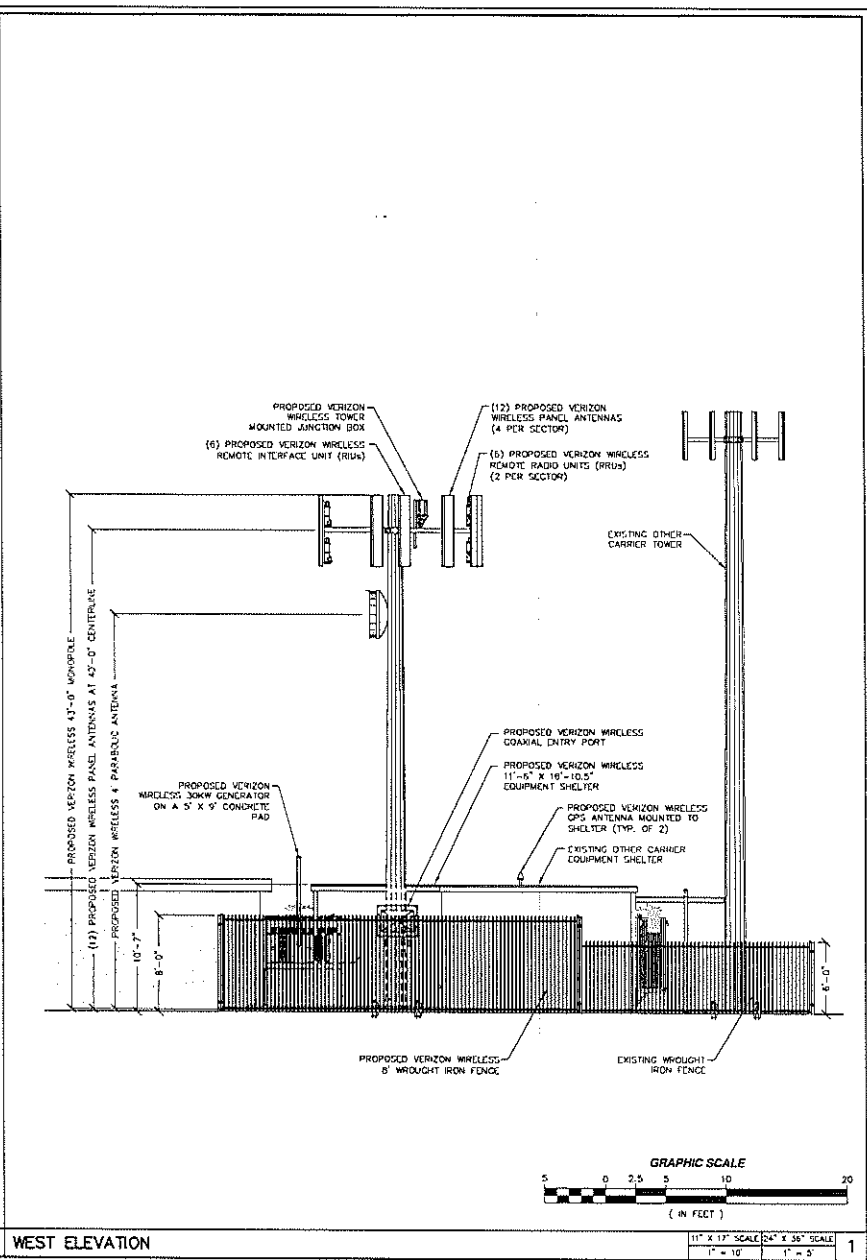
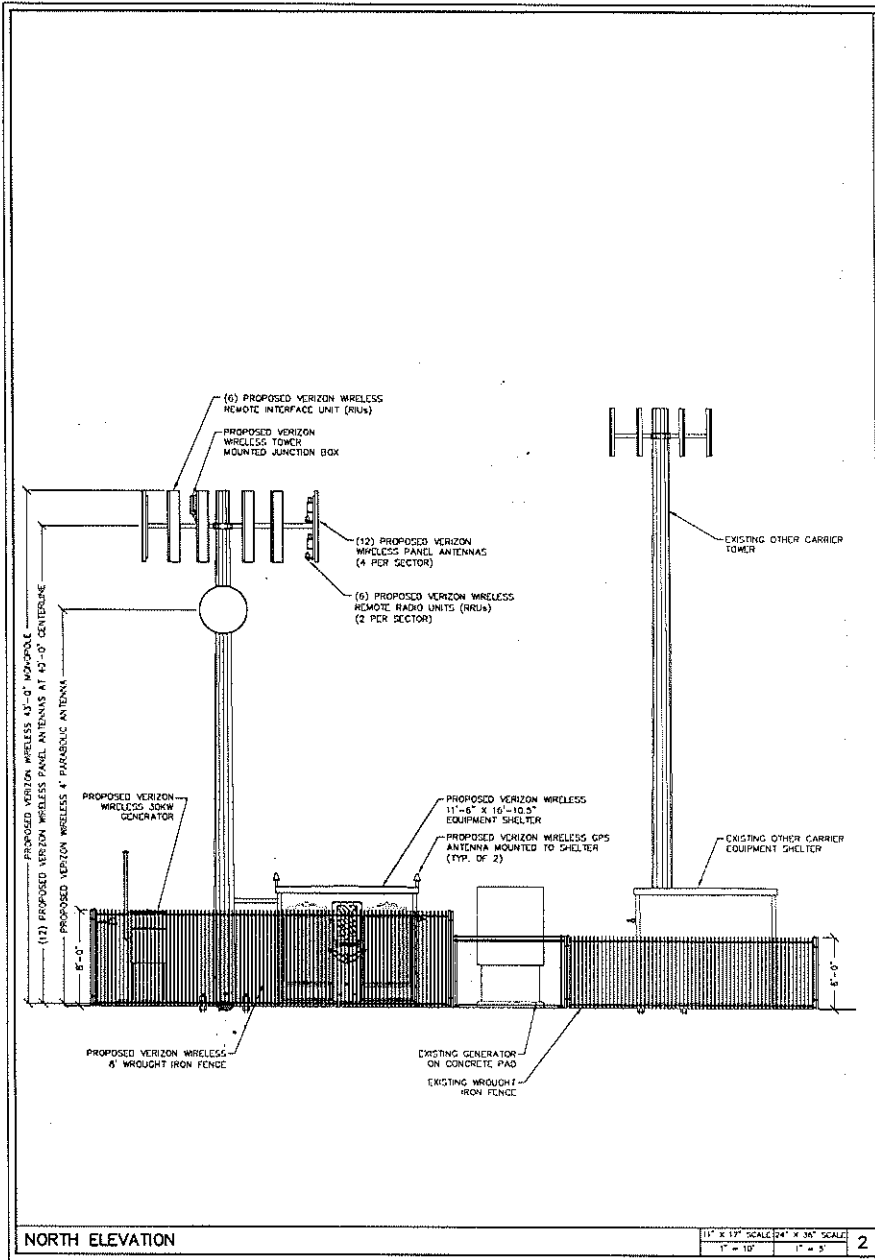


**EAST ELEVATION**



11" x 17" SCALE 24" x 36" SCALE  
 1" = 10' 1" = 5'





**verizon**wireless  
15500 SAND CANYON AVENUE  
BUILDING D, 127 FLOOR  
IRVINE, CALIFORNIA 92618

PROJECT INFORMATION  
**HOLLYHOCK**  
3601 MESQUITE AVENUE  
PALM SPRINGS, CALIFORNIA 92264

CURRENT ISSUE DATE:  
**12/03/13**

ISSUED FOR:  
**ZONING**

REV.: DATE: DESCRIPTION: BY:

REV.	DATE	DESCRIPTION	BY
1	12/03/13	100% ZONING	T.R.
2	11/26/13	90% ZONING	R.C.

PLANS PREPARED BY:  
**SPECTRUM**  
SPECTRUM SERVICES, INC.  
8390 MAPLE PLACE, SUITE 110  
RANCHO CUCAMONGA, CA 91730  
PH. (866) 515-3358  
FAX (666) 515-3359

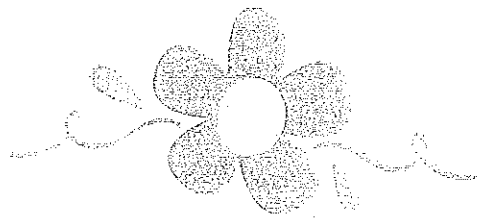
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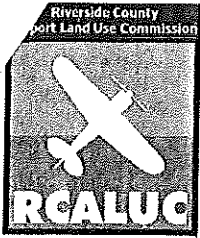
SHEET TITLE:  
**WEST AND NORTH ELEVATIONS**

SHEET NUMBER: REVISION:

**A4** 1  
LAX-299

**PAGE BREAK**





# AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

**CHAIR**

Simon Housman  
Rancho Mirage

March 31, 2014

**VICE CHAIRMAN**

Rod Ballance  
Riverside

Mr. Jay Wu, Associate Planner  
City of La Quinta Planning Division  
78-495 Calle Tampico  
La Quinta, CA 92253

**COMMISSIONERS**

Arthur Butler  
Riverside

John Lyon  
Riverside

Glen Holmes  
Hemet

Greg Pettis  
Cathedral City

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW**  
File No.: ZAP1054BD14  
Related File No.: SDP 14-940  
APN: 609-052-002

Richard Stewart  
Moreno Valley

Dear Mr. Wu:

**STAFF**

Director  
Ed Cooper

John Guerin  
Russell Brady  
Barbara Santos

Under the delegation of the Riverside County Airport Land Use Commission (ALUC), staff reviewed the above-referenced proposal for a site development plan for 19 single-family residences on 19 lots located southerly of Darby Road and easterly of Palm Royale Drive in the City of La Quinta. The project is associated with Tentative Tract Map No. 31087, which is presently in the final map process, awaiting recordation. Tentative Tract Map No. 31087 was approved in 2004, prior to the adoption of the current Bermuda Dunes Airport Land Use Compatibility Plan.

County Administrative Center  
4080 Lamon St., 14<sup>th</sup> Floor  
Riverside, CA 92501  
(951) 955-5132

[www.rcaluc.org](http://www.rcaluc.org)

The site is located in Airport Compatibility Zone E of the 2004 Bermuda Dunes Airport Land Use Compatibility Plan. Based on the distance of the parcel to the end of the runway at 7,900 feet and the elevation of the westerly end of the runway at 73.4 feet above mean sea level (AMSL), FAA Obstruction Evaluation review would be required for any structure with a top point elevation exceeding 152.4 feet AMSL. The maximum building height proposed is 27 feet, 8 inches (27.67 feet). Provided that the pad elevations do not exceed 124.7 feet above mean sea level, the top point elevation of 152.4 feet AMSL will not be exceeded, and FAA Obstruction Evaluation review for height/elevation reasons will not be required.

I hereby find the above-referenced project **CONSISTENT** with the 2004 Bermuda Dunes Airport Land Use Compatibility Plan, subject to the following conditions:

**CONDITIONS:**

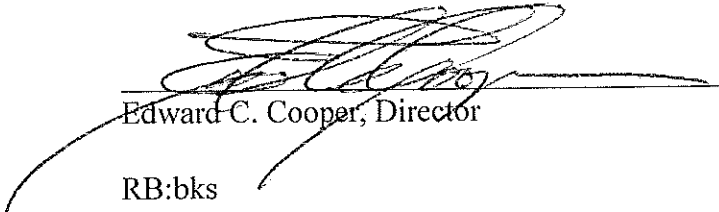
1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:

- (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, artificial marshes, recycling centers containing putrescible wastes, and construction and demolition debris facilities.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Any highly noise-sensitive outdoor nonresidential uses and hazards to flight.
3. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and/or tenants of the properties.
  4. The top point elevations of each unit shall not exceed 152.4 feet above mean sea level. In order to assure compliance with this provision, the maximum pad elevation of lots where two-story structures (or other structures 20 feet or greater in height) are proposed shall not exceed 124.7 feet above mean sea level.
  5. The retention basin shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the retention basin that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping. On-site landscaping shall not include trees that produce seeds, fruits, or berries. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature.

If you have any questions, please contact Russell Brady, Contract Planner, at (951) 955-0549, or John Guerin, Principal Planner, at (951) 955-0982.

March 31, 2014

Sincerely,  
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Edward C. Cooper, Director

RB:bks

Attachment: Notice of Airport in Vicinity

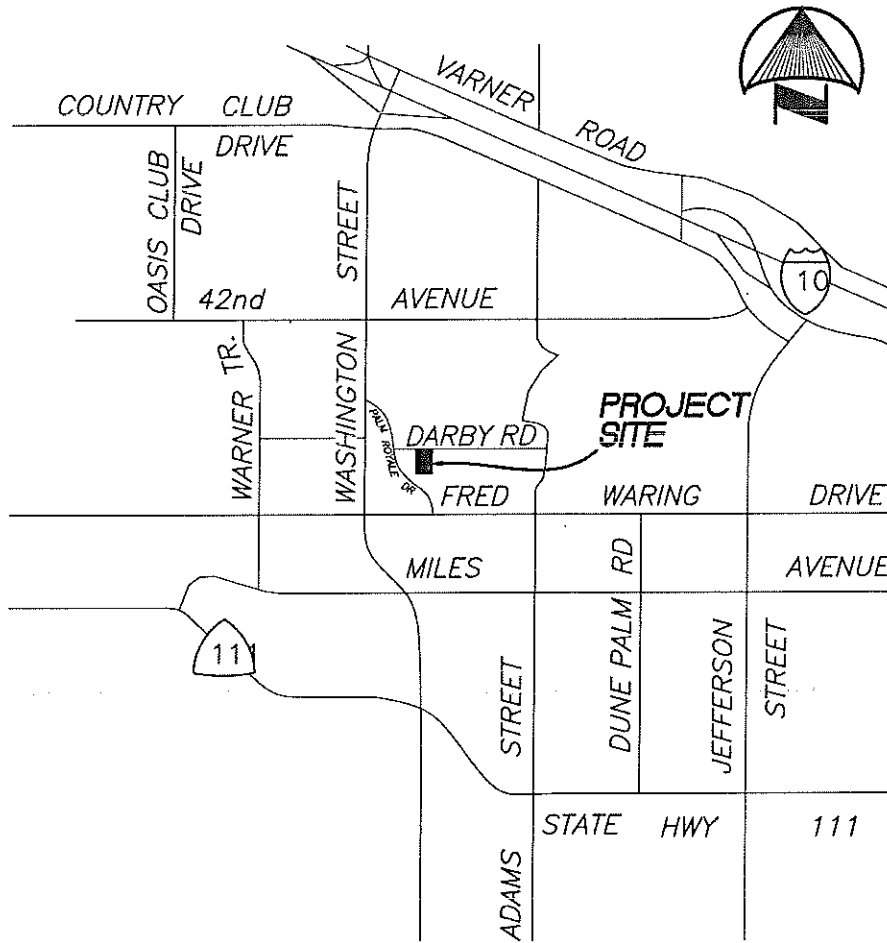
cc: Adom Ventures, Steve Hyman  
GHA Enterprises, Mario Gonzales  
Mike Smith, Manager, Bermuda Dunes Executive Airport  
ALUC Staff

Y:\AIRPORT CASE FILES\Bermuda Dunes\ZAP1054BD14\ZAP1054BD14.LTR.doc



# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

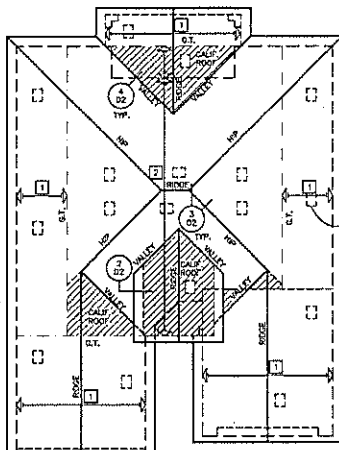


**VICINITY MAP**

NO SCALE

THOMAS BROS. COORDINATES - PG. 819, F-6  
2012 EDITION





**ROOF PLAN**  
SCALE: 1/8"=1'-0"

**ROOFING:**

CONCRETE TILE ROOF.  
SEE CN-213, TYP.  
ROOF ASSEMBLY SHALL HAVE A CLASS "70" FIRE RATING.

**ROOF FRAMING:**

- DESIGNED WOOD TRUSSES @24"oc (2-4" POINT BEARING).
- DESIGNED WOOD TRUSSES @24"oc (MULTI-POINT BEARING).

**NOTE:**

- ALL PITCHES 4:12.
- ALL EAVE OVERHANGS 18" U.N.D.
- ALL RAKE OVERHANGS 12" U.N.D.

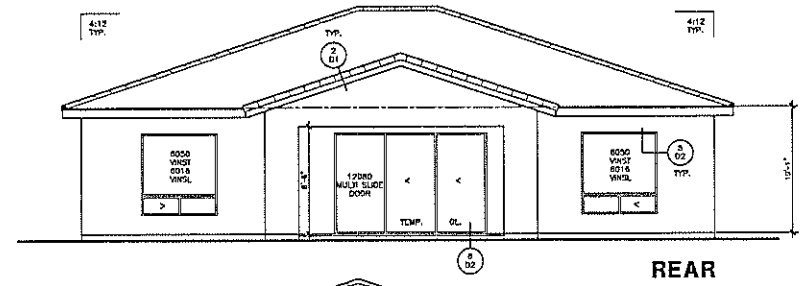
**FINISHES:**

- ALL STUCCO SHALL BE SAND FLOT FINISH.
- ALL "STUCCO TRIM" REFER TO FOAM SHAPE ADHERED TO STUCCO BROWN COAT AND FINISHED W/ COLOR COAT OF APPROVED BRANDS OR APPROVED FISH-REINFORCED BASE COAT.
- SEE COLOR CONSULTANT'S SPECIFICATIONS FOR COLOR APPLICATION, ROOF TILE AND MASONRY SELECTIONS.

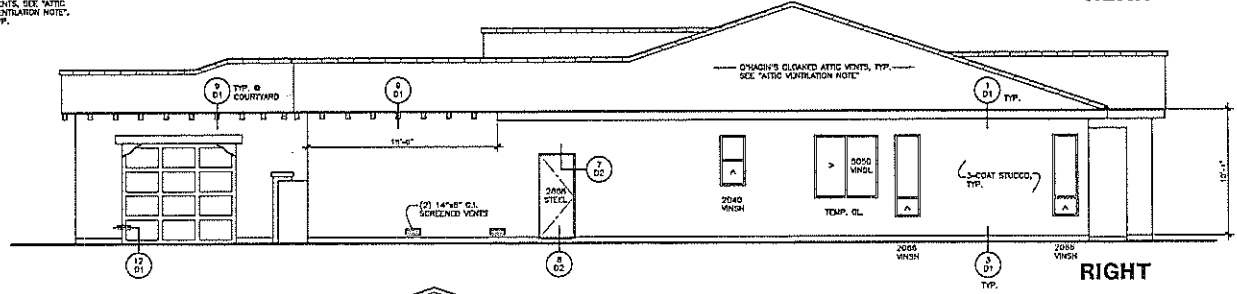
**ATTIC VENTILATION NOTE:**

- TOTAL ATTIC AREA 3420 SQ. FT. (HOUSE + GARAGE + PORCHES) = 300 = 11.42 SQ. FT. (1142 SQ. IN.) OF NET FREE VENTILATING AREA REQUIRED.
- USE CHAZIN CLOAKED VENT TILES INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S INSTALLATION RECOMMENDATIONS. (800-23 9850A, www.enclivent.com)
- MIN. 40% / MAX. 50% OF THE REQUIRED VENTILATION SHALL BE LOCATED IN THE UPPER PORTION OF THE ATTIC NO MORE THAN 3 FT. BELOW THE ROOF, WITH THE BALANCE LOCATED AT THE GARAGE IMMEDIATELY ABOVE THE INSULATION LAYER.
- THE VENTS SHALL BE LOCATED IN A MANNER GENERATING CROSS VENTILATION IN THE ATTIC AREA.
- \* BASED ON CHAZIN VENTS WITH A NET FREE VENTILATING AREA OF 88.25 SQUARE INCHES, USE A MINIMUM TOTAL OF 12 VENTS. SEE CN-10.0.1 FOR FURTHER INFORMATION.

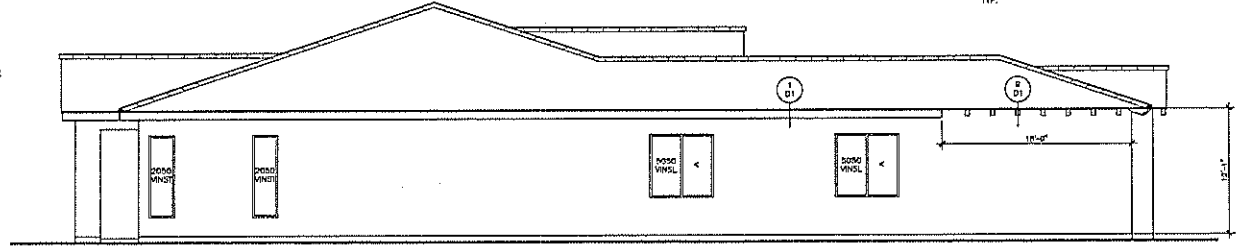
10 HIGH x 8 LOW 88.25 SQ. IN. CHAZIN VENTS, SEE "ATTIC VENTILATION NOTE", TYP.



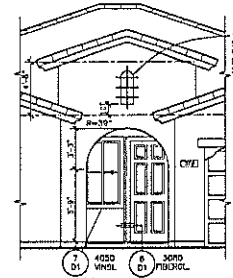
**REAR**



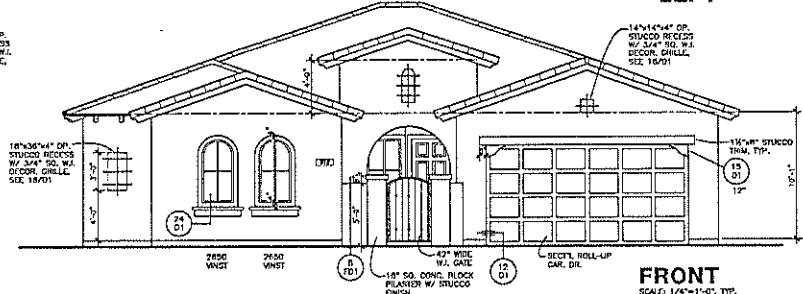
**RIGHT**



**LEFT**



**COURTYARD**



**FRONT**  
SCALE: 1/4"=1'-0", TYP.

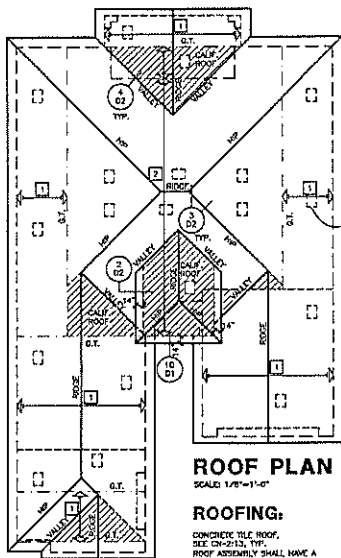
19 @ DARBY  
Tract 31087, La Quinta, CA

**LEN NOBEL CO.**  
P.O. BOX 18005, IRVINE, CALIF. 92613  
PHONE: (949) 850-4305 www.lnobl.com



**ELEVATION A**  
**PLAN 1**

**SHEET**  
**6**



**ROOF PLAN**  
SCALE: 1/8"=1'-0"

**ROOFING:**

CONCRETE TILE ROOF,  
SEE CH-213, TYP.  
ROOF ASSEMBLY SHALL HAVE A  
CLASS "A" FIRE RATING.

**ROOF FRAMING:**

- DESIGNED WOOD TRUSSES @24"oc (2-POINT BEARING).
- DESIGNED WOOD TRUSSES @24"oc (MULTI-POINT BEARING).

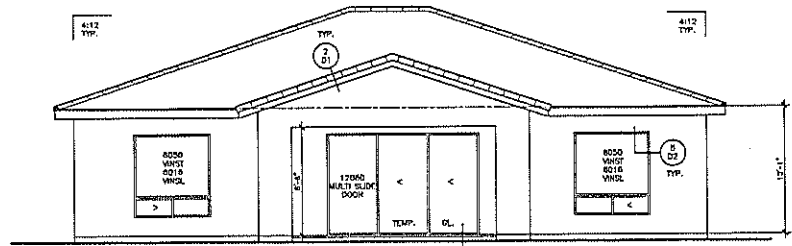
**NOTE:**

- ALL PITCHES 4:12.
- ALL EAVE OVERHANDS 18" O.A.O.
- ALL RAKE OVERHANDS 12" O.A.O.

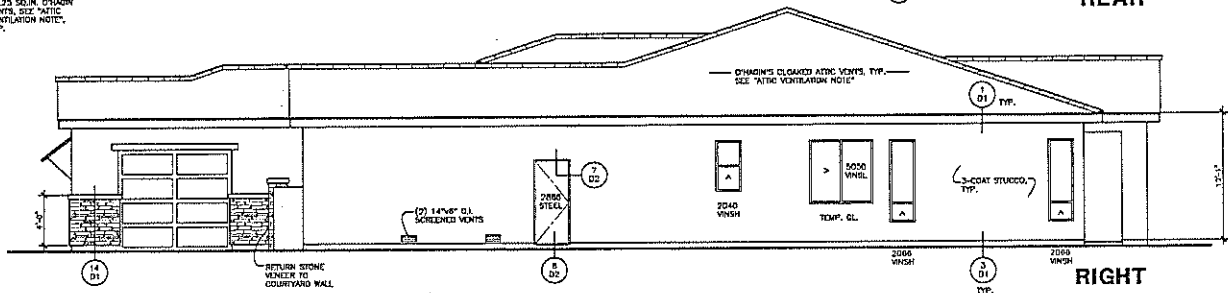
**ATTIC VENTILATION NOTE:**

- TOTAL ATTIC AREA: 3420 SQ. FT. (HOUSE + GARAGE + PORCHES) + 300 = 1140 SQ. FT. (1642 SQ. IN.) OF NET FREE VENTILATING AREA REQUIRED.
- USE CHIMNEY CLOAKED VENT TILES INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S INSTALLATION RECOMMENDATIONS. (100-23 9820A, www.chimney.com)
- MIN. 40% / MAX. BOX OF THE REQUIRED VENTILATION SHALL BE LOCATED IN THE UPPER PORTION OF THE ATTIC NO HIGHER THAN 3 FT. BELOW THE RIDGE, WITH THE BALANCE LOCATED AT THE COURSES IMMEDIATELY ABOVE THE INSULATION LAYERS.
- THE VENTS SHALL BE LOCATED IN A MANNER GENERATING CROSS VENTILATION IN THE ATTIC AREA.
- BASED ON CHIMNEY VENTS WITH A NET FREE VENTILATING AREA OF 89.25 SQUARE INCHES, USE A MINIMUM TOTAL OF 1623 INCHES. (NOTE: SEE CH-118.1 FOR FURTHER INFORMATION.)

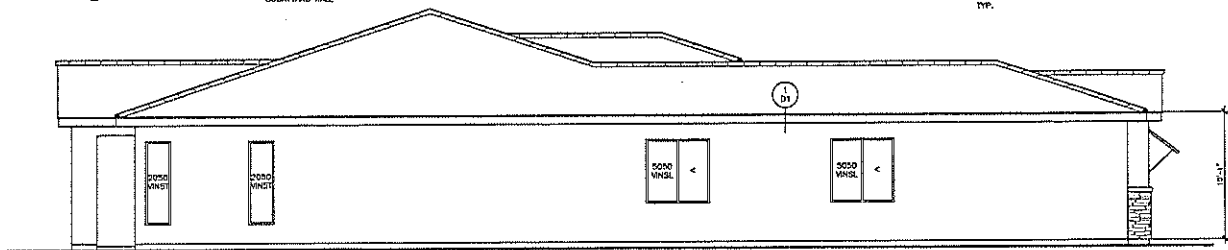
18 HIGH x 8 LOW  
1623 SQ. IN. CHIMNEY  
VENTS, SEE "ATTIC  
VENTILATION NOTE",  
TYP.



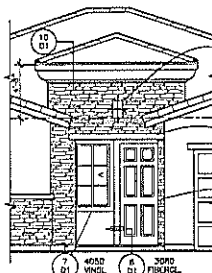
**REAR**



**RIGHT**



**LEFT**



**COURTYARD**



**FRONT**

**FINISHES:**

- ALL STUCCO SHALL BE SAND FLAT FINISH.
- ALL "STUCCO TRIM" REFER TO FORM SHAPE ADHERED TO STUCCO BROWN COAT AND FINISHED BY COLOR COAT OF APPROVED KINDS OF APPROVED FIBER-REINFORCED PLASTER OR CONCRETE.
- SEE COLOR CONSULTANT'S SPECIFICATIONS FOR COLOR APPLICATION, ROOF TILE AND MASONRY SELECTIONS.

19 @ DARBY  
Tract 31097, La Quinta, CA

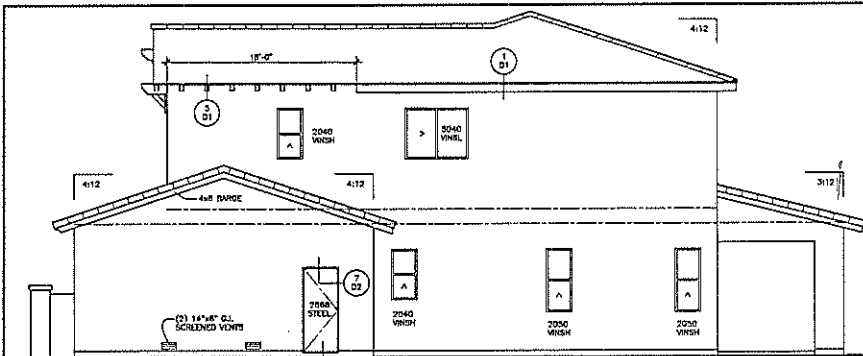
**LEN NOBEL CO.**  
P.O. BOX 14808, BURNING WOOD, CA 92615  
PHONE: (949) 588-4305 www.lennobel.com

2012.10.10.dwg

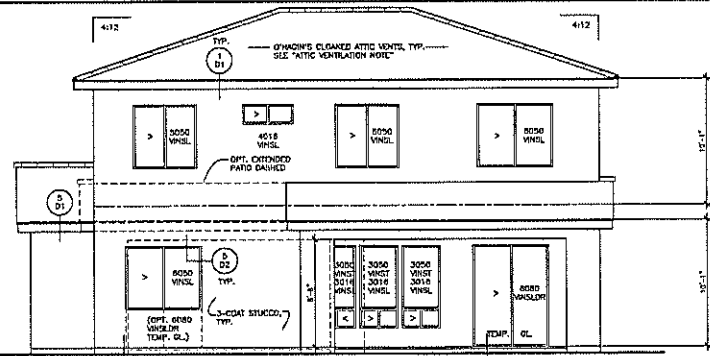
**ELEVATION B**  
**PLAN 1**

SHEET  
7

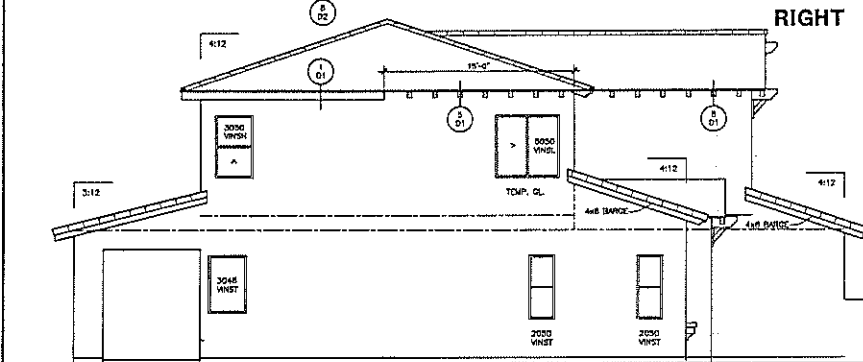




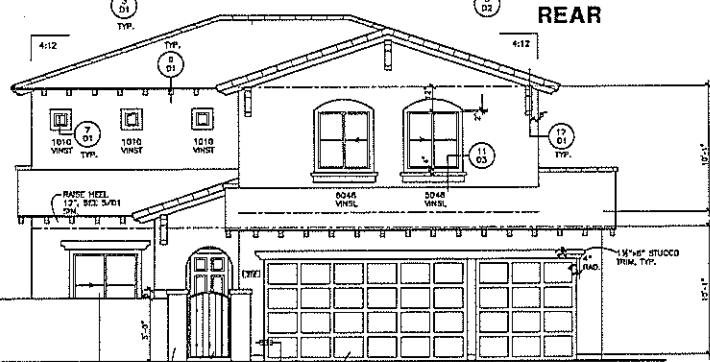
RIGHT



REAR



LEFT



FRONT

SCALE: 1/4"=1'-0", TYP.

**ATTIC VENTILATION NOTE:**

- USE OSBORN CLEANED VENT TILES INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S INSTALLATION RECOMMENDATIONS. (ICC-ES E3836A, www.intelgreen.com)
- MIN. 40% / MAX. 50% OF THE REQUIRED VENTILATION SHALL BE LOCATED IN THE UPPER PORTION OF THE ATTIC NO MORE THAN 3 FT. BELOW THE RIDGE, WITH THE BALANCE LOCATED AT TILE COURSES IMMEDIATELY ABOVE THE INSULATION LAYER.
- THE VENTS SHALL BE LOCATED IN A MANNER GENERATING CROSS VENTILATION IN THE ATTIC AREA.

**UPPER FLOOR ATTIC:**

- TOTAL ATTIC AREA: 1478 SQ.FT.
- + 300 = 1778 SQ.FT. (400 SQ.FT.) OF NET FREE VENTILATING AREA REQUIRED.
- BASED ON OSBORN VENTS WITH A NET FREE VENTILATING AREA OF 99.25 SQUARE INCHES, SEE A MINIMUM TOTAL OF 2 VENTS.

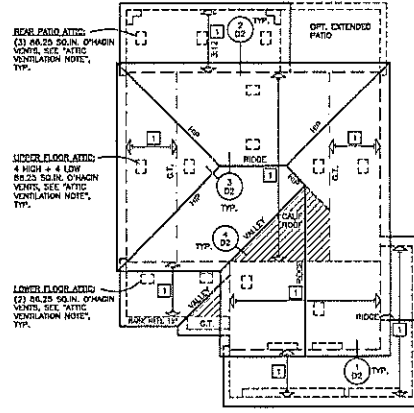
**LOWER FLOOR ATTIC:**

- TOTAL ATTIC AREA: 129 SQ.FT.
- + 150 = 144 SQ.FT. (229 SQ.FT.) OF NET FREE VENTILATING AREA REQUIRED.
- BASED ON OSBORN VENTS WITH A NET FREE VENTILATING AREA OF 99.25 SQUARE INCHES, SEE A MINIMUM TOTAL OF 2 VENTS.

**REAR PATIO ATTIC:**

- TOTAL ATTIC AREA: 237 SQ.FT.
- + 150 = 387 SQ.FT. (239 SQ.FT.) OF NET FREE VENTILATING AREA REQUIRED.
- BASED ON OSBORN VENTS WITH A NET FREE VENTILATING AREA OF 99.25 SQUARE INCHES, SEE A MINIMUM TOTAL OF 2 VENTS.

NOTE: SEE CH 15.8.1 FOR FURTHER INFORMATION.



**ROOFING:**

CONCRETE TILE ROOF, SILE CR-213, TYP. ROOF ACCESSORY SHALL HAVE A CLASS "A" FIRE RATING.

**ROOF FRAMING:**

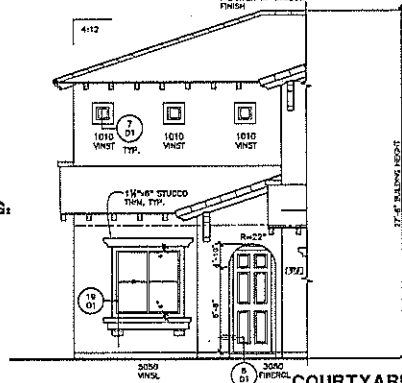
- DESIGNED WOOD TRUSSES @24"oc (2-POINT BEARING).
- DESIGNED WOOD TRUSSES @24"oc (3-POINT BEARING).

**NOTE:**

- ALL PITCHES 4:12 U.N.O.
- ALL EAVE OVERHANGS 18" U.N.O.
- ALL RAKE OVERHANGS 12" U.N.O.

**ROOF PLAN**

SCALE: 1/8"=1'-0"



COURTYARD

**FINISHES:**

- ALL STUCCO SHALL BE SAND FLOAT FINISH.
- ALL "STUCCO TRIM" REFER TO FOAM SHAPE ADHERED TO STUCCO BROWN COAT AND FINISHED W/ COLOR COAT OF APPROPRIATE BEHAVE OR APPROVED FIBER-REINFORCED GAGE COAT.
- SEE COLOR CONSULTANT'S SPECIFICATIONS FOR COLOR APPLICATION, ROOF TILE AND MASONRY SELECTIONS.

19 @ DARBY  
Tract 31087, La Quinta, CA

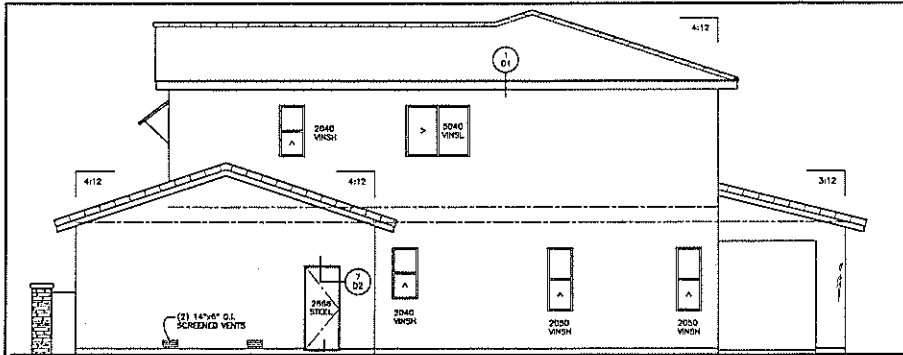
LEN NOBEL CO.  
P.O. BOX 18408, IRVINE, CALIF. 92623  
PHONE: (949) 568-4308 www.lenobel.com



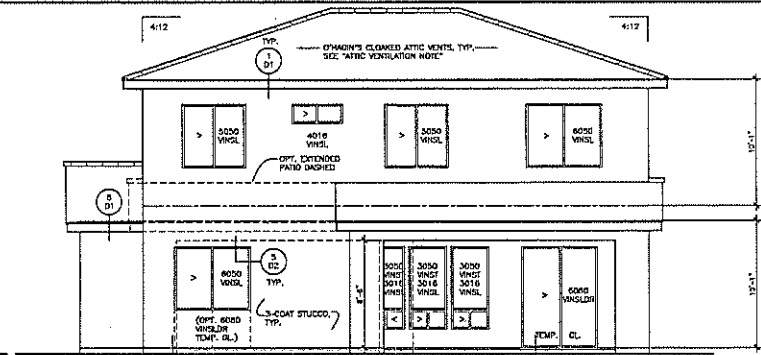
30211A10.dwg

ELEVATION A  
PLAN 2

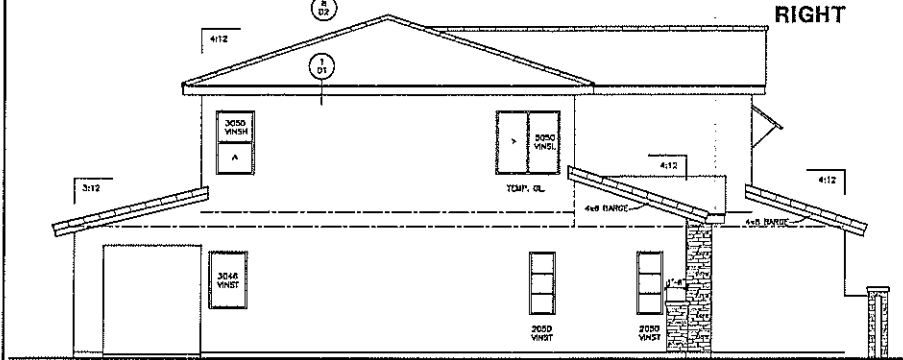
2:  
6



RIGHT



REAR



LEFT



FRONT

**ATTIC VENTILATION NOTE:**

- USE OHVAIN CLOAKED VENT TILES INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S INSTALLATION RECOMMENDATIONS. (SEEKER B050A - [www.poppovent.com](http://www.poppovent.com))
- MIN. ONE (1) MAX. ONE (1) OF THE REQUIRED VENTILATION SHALL BE LOCATED IN THE UPPER PORTION OF THE ATTIC AND MORE THAN 3 FT. BELOW THE RIDGE WITH THE BALANCE LOCATED AT THE COURTESIES IMMEDIATELY ABOVE THE INSULATION LAYER.
- THE VENTS SHALL BE LOCATED IN A MANNER GENERATING CROSS VENTILATION IN THE ATTIC AREA.

**UPPER FLOOR ATTIC:**

- TOTAL ATTIC AREA: 1418 SQ.FT.
- 1500 = 1,225 SQ.FT. (131 SQ.IN.) OF NET FREE VENTILATING AREA REQUIRED.
- BASED ON OHVAIN VENTS WITH A NET FREE VENTILATING AREA OF 26.25 SQUARE INCHES, USE A MINIMUM TOTAL OF 8 VENTS.

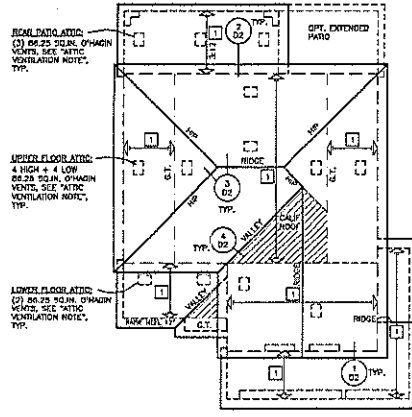
**LOWER FLOOR ATTIC:**

- TOTAL ATTIC AREA: 186 SQ.FT.
- 1500 = 1,225 SQ.FT. (131 SQ.IN.) OF NET FREE VENTILATING AREA REQUIRED.
- BASED ON OHVAIN VENTS WITH A NET FREE VENTILATING AREA OF 88.25 SQUARE INCHES, USE A MINIMUM TOTAL OF 2 VENTS.

**REAL DATA ATTIC:**

- TOTAL ATTIC AREA: 237 SQ.FT.
- 1500 = 1,225 SQ.FT. (131 SQ.IN.) OF NET FREE VENTILATING AREA REQUIRED.
- BASED ON OHVAIN VENTS WITH A NET FREE VENTILATING AREA OF 88.25 SQUARE INCHES, USE A MINIMUM TOTAL OF 3 VENTS.

NOTE: SEE CHY1.A.B.1 FOR FURTHER INFORMATION.



**ROOFING:**

CONCRETE TILE ROOF, SEE CH-213, TYP. ROOF ASSEMBLY SHALL HAVE A CLASS 'A' FIRE RATING.

**ROOF FRAMING:**

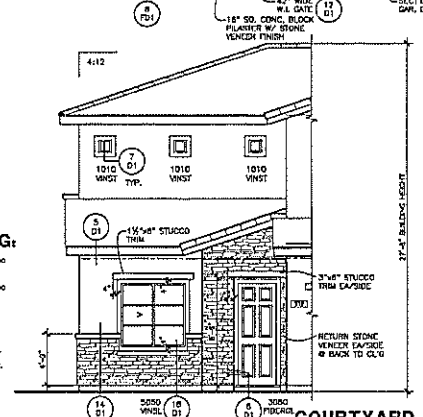
1. DESIGNED WOOD TRUSSES @24"OC (2-POINT BEARING).
2. DESIGNED WOOD TRUSSES @24"OC (3-POINT BEARING).

NOTE:

- ALL PITCHES 4:12 U.N.O.
- ALL EAVE OVERHANGS 18" U.N.O.
- ALL RAKE OVERHANGS 12" U.N.O.

**ROOF PLAN**

SCALE: 1/8"=1'-0"



COURTYARD

**FINISHES:**

- ALL STUCCO SHALL BE SAND FLOAT FINISH.
- ALL STUCCO TRIM REFER TO FOAM SHAPY, ADHERED TO STUCCO BROWN COAT AND FINISHED W/ COLOR COAT OF APPROVED BRAND OR APPROVED FIRE-REINFORCED BASE COAT.
- SEE COLOR CONSULTANT'S SPECIFICATIONS FOR COLOR APPLICATION, ROOF TILE AND MASONRY SELECTIONS.

19 © DARBY  
Tract 31087, La Quinta, CA

LEN NOBEL CO.  
P.O. BOX 16606, IRVINE, CALIF. 92623  
PHONE: (949) 669-4305 [www.nobel.net](http://www.nobel.net)



ELEVATION B  
PLAN 2  
SHEET 7

EXCERPTS



# CALIFORNIA AIRPORT LAND USE CONSORTIUM CONFERENCE PROGRAM

March 19 – 21, 2014  
Rohnert Park, California





## AGENDA AT A GLANCE

Wednesday, March 19

### SESSION 1

1:15 pm - 1:30 pm	Opening Remarks
1:30 pm - 2:45 pm	Preparing an ALUCP
2:45 pm - 3:30 pm	CEQA Issues
3:30 pm - 3:45 pm	Afternoon Break
3:45 pm - 5:00 pm	ALUCP Adoption Process
5:00 pm - 6:00 pm	Welcome Reception with Caltrans Staff

Thursday, March 20

### SESSION 2

7:30 am - 8:00 am	Continental Breakfast
8:00 am - 8:15 am	Opening Remarks
8:15 am - 9:45 am	How Airports Affect Their Environs and Vice Versa
9:45 am - 10:00 am	Morning Break
10:00 am - 11:45 am	FAA Guidance on Compatibility Issues
11:45 am - 1:15 pm	Lunch Buffet – Keynote Speaker

### SESSION 3

1:15 pm - 2:00 pm	Caltrans Role in Compatibility Planning
2:00 pm - 2:45 pm	ALUC Financial Issues
2:45 pm - 3:00 pm	Afternoon Break
3:00 pm - 4:00 pm	Legal Briefing
4:00 pm - 5:00 pm	The Future – Open Discussion
5:00 pm - 6:00 pm	Reception Hosted by ALUC Consultants

Friday, March 21

### SESSION 4

7:30 am - 8:00 am	Continental Breakfast
8:00 am - 8:45 am	Consortium Business
8:45 am - 10:00 am	Compatibility Reviews and ALUCP Implementation – Challenging Issues
10:00 am - 10:30 am	Morning Break
10:30 am - Noon	Compatibility Reviews and ALUCP Implementation – cont'd
Noon - 12:15 pm	Ending Acknowledgements

## KEYNOTE SPEAKER LEWIS MICHAELSON



Lewis Michaelson has worked nearly 30 years designing and facilitating consensus-building programs for environmental, planning and impact analysis projects. He is a Senior Vice President and corporate officer of Katz & Associates and leads efforts in their San Diego office.

Using a collaborative problem solving approach, Lewis has facilitated community outreach and consensus for projects and issues involving a wide range of subjects, including transportation planning, energy production and transmission, wastewater and stormwater projects, airport facilities, regional water supply facilities, hazardous waste cleanup, watershed planning, military base closures and expansions and land use management plans. His keynote presentation will focus on how to promote collaboration with your stakeholders for better decision-making – and how to move forward if they won't or can't participate in a collaborative process.

In addition to his project and case work, Lewis is the past president of the International Association for Public Participation. As a board member of IAP2, he was instrumental in spearheading the development of the Core Values for the Practice of Public Participation and the IAP2 Code of Ethics. He was also one of the three original developers of the Foundations of Public Participation training courses instituted by IAP2. Lewis is also a qualified member of the U.S. Institute for Environmental Conflict Resolution Roster of Neutrals. He has a BA in sociology and an MS in conflict management.

# Conference Agenda



## Session 1: Wednesday Afternoon, March 19 – ALUCP PREPARATION AND ADOPTION

1:15 – 1:30	<p>Opening Remarks and Acknowledgements: Welcome and Introductions from the Conference Hosts</p>	<p><b>Moderator:</b> Ken Brody, Mead &amp; Hunt</p> <p><b>Speaker:</b> Mike Swearingen, San Joaquin Council of Governments</p>
1:30 – 2:45	<p>Session 1A: Preparing an ALUCP Airport Land Use Compatibility Plans have become increasingly detailed and complex over the years — what are the steps involved in preparing these plans and what hurdles should be anticipated?</p>	<p><b>Moderator:</b> Cindy Horvath, Alameda County Airport Land Use Commission</p> <p><b>Speakers:</b> Dave Fitz, Coffman Associates John Guerin, Riverside County Airport Land Use Commission Adrian Jones, ESA</p>
2:45 – 3:30	<p>Session 1B: CEQA Issues The Muzzy Ranch decision dictates that ALUCs prepare a CEQA document prior to adopting or amending an ALUCP — what does this analysis involve?</p>	<p><b>Moderator:</b> Mike Swearingen, San Joaquin Council of Governments</p> <p><b>Speakers:</b> Maranda Thompson, Mead &amp; Hunt Phil Wade, ESA</p>
3:30 – 3:45	Afternoon Break	
3:45 – 5:00	<p>Session 1C: ALUCP Adoption Process A wide range of stakeholders usually have an interest in the content of an ALUCP — what are the requirements and best practices for managing a successful outreach process?</p>	<p><b>Moderator:</b> Angie Jamison, San Diego County Regional Airport Authority</p> <p><b>Speakers:</b> Dave Carbone, San Mateo County Airport Land Use Commission Amy Gonzalez, San Diego County Regional Airport Authority Dan Landon, Nevada County Transportation Commission</p>
5:00 – 6:30	Welcome Reception with Cal-ALUC and Caltrans: Casual Reception with Appetizers and No-Host Bar	



## Session 2: Thursday Morning, March 20 – AIRPORT AND COMPATIBILITY PLANNING BASICS

7:30 – 8:00	Continental Breakfast	
8:00 – 8:15	Opening Remarks	<p><b>Moderator:</b> Ken Brody, Mead &amp; Hunt</p> <p><b>Speakers:</b> Steve Dial, San Joaquin Council of Governments Gary Cathey, Caltrans Division of Aeronautics</p>
8:15 – 9:45	<p>Session 2A: How Airports Affect Their Environs and Vice Versa</p> <p>Airports and ALUCPs are inexorably intertwined — what do preparers and users of ALUCPs need to know about how airports are designed and operated?</p>	<p><b>Moderator:</b> Dave Fitz, Coffman Associates</p> <p><b>Speakers:</b> Rick Beach, Pilot Dan Card, Fresno County Airport Land Use Commission Austin Wiswell, Pilot</p>
9:45 – 10:00	Morning Break	
10:00 – 11:45	<p>Session 2B: FAA Guidance on Compatibility Issues</p> <p>The Federal Aviation Administration has a well-defined role in many aspects of airport design and land use compatibility— what do the preparers and users of ALUCPs need to know about the agency's guidance?</p>	<p><b>Moderator:</b> John Guerin, Riverside County Airport Land Use Commission</p> <p><b>Speakers:</b> Brian Armstrong/Margie Drilling, FAA Lisa Harmon/Corbett Smith, Mead &amp; Hunt Mark Johnson, Ricondo &amp; Associates Chris Jones, ATAC Corporation</p>
11:45 – 1:15	Lunch Buffet – provided / Keynote Speaker	

## Session 3: Thursday Afternoon, March 20 – LEGAL AND FINANCIAL TOPICS

1:15 – 2:00	<p>Session 3A: Caltrans Role in Compatibility Planning</p> <p>Through various state statutes and the Handbook guidance, Caltrans plays a central role in airport land use compatibility planning — how does this role need to factor into the compatibility planning process in your county?</p>	<p><b>Moderator:</b> Lisa Harmon, Mead &amp; Hunt</p> <p><b>Speakers:</b> Raiyn Bain-Moore, Caltrans Legal Ron Bolyard, Caltrans Division of Aeronautics Derek Kantar, Caltrans Division of Aeronautics</p>
2:00 – 2:45	<p>Session 3B: ALUC Financial Issues</p> <p>Preparation of ALUCPs and associated CEQA documents are expensive endeavors and the review of individual land use projects takes time and money, too — how do we pay for compatibility planning?</p>	<p><b>Moderator:</b> Mark Bautista, Monterey Peninsula Airport</p> <p><b>Speakers:</b> Terry Barrie, Caltrans Division of Aeronautics Steve Dial, San Joaquin Council of Governments</p>
2:45 – 3:00	Afternoon Break	
3:00 – 4:00	<p>Session 3C: Legal Briefing</p> <p>Much of what ALUCs do is determined by statutes and case law — what issues are in the forefront today?</p>	<p><b>Moderator:</b> Dan Landon, Nevada County Transportation Commission</p> <p><b>Speakers:</b> Raiyn Bain-Moore, Caltrans Legal Amy Gonzalez, San Diego County Regional Airport Authority Christy Layton, Miller &amp; Owen</p>
4:00 – 5:00	<p>Session 3D: Airport Land Use Compatibility Planning / Open Discussion</p> <p>It is almost 50 years since ALUCs first came into existence in California — what have we learned and accomplished from this experience and where are we headed in the future?</p>	<p><b>Moderator:</b> Dave Carbone, San Mateo County Airport Land Use Commission</p> <p><b>Speakers:</b> Gary Cathey, Caltrans Division of Aeronautics Simon Housman, Riverside County Airport Land Use Commission Angie Jamison, San Diego County Regional Airport Authority</p>
5:00 – 6:30	Reception Hosted by Cal-ALUC and Conference Sponsors: Casual Reception with Appetizers and No-Host Bar	



# Conference Agenda



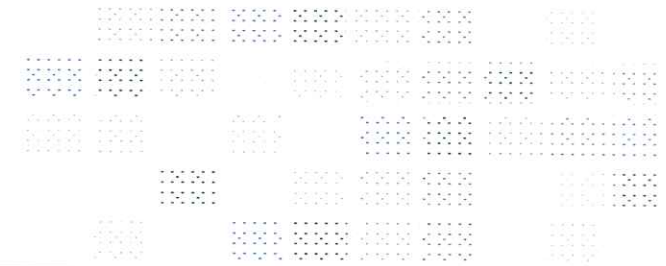
## Session 4: Friday Morning, March 21 – CONSORTIUM BUSINESS / PROJECT REVIEWS

7:30 – 8:00	Continental Breakfast	
8:00 – 8:45	<p><b>Session 4A: ALUC Consortium Business</b> This is now the second conference of the ALUC Consortium — how do we best keep the ball rolling in the future at conferences and in between?</p>	<p><b>Moderator:</b> Laural Fawcett, Fresno Council of Governments</p> <p><b>Speakers:</b> Laura Brunn, San Joaquin Council of Governments Mark Bautista, Monterey Peninsula Airport</p>
8:45 – 10:00	<p><b>Session 4B, Part 1: Compatibility Reviews and ALUCP Implementation – Challenging Issues</b> Where the rubber meets the runway in the compatibility planning process is in the review of individual local plans and development proposals — what difficult issues have ALUCs faced and how have they been resolved?</p>	<p><b>Moderator:</b> Simon Housman, Riverside County Airport Land Use Commission</p> <p><b>Speakers:</b> Marnie Delgado, City of Dublin Ed Gowens, San Diego County Airport Land Use Commission Cindy Horvath, Alameda County Airport Land Use Commission Nick Johnson, Johnson Aviation Nathan Magsig, Fresno County Airport Land Use Commission Lorena Mejia, City of Ontario Tony Whitehurst, Merced County Airport Land Use Commission</p>
10:00 – 10:30	Morning Break – and opportunity to check out of your room	
10:30 – 12:00	<p><b>Session 4B Continued:</b> There is so much to discuss in this topic, that we will continue after the morning break</p>	
12:00 – 12:15	Ending Acknowledgements	<p><b>Moderator:</b> Ken Brody, Mead &amp; Hunt</p> <p><b>Speaker:</b> Steve Dial, San Joaquin Council of Governments</p>



2012 Conference Highlights





AIRPORT LAND USE CONSORTIUM CONFERENCE

Airport Land Use Compatibility Plan Preparation – Guidance from the Handbook

## Session 1A: Preparing an ALUCP

John Guerin  
Riverside County Transportation & Land Management Agency  
Airport Land Use Commission Staff  
March 19, 2014



# Types of Compatibility Concerns

AIRPORT LAND USE CONSORTIUM CONFERENCE

## Types of Compatibility Concerns

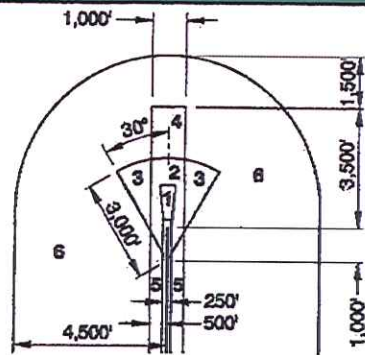
- **Safety**
- **Airspace Protection**
- **Noise**
- **Overflight**



## Safety: The Geography of Risk

“Safety is in many respects a more difficult concern to address in compatibility policies than noise [largely because] safety policies address uncertain events that may occur with occasional aircraft operations, whereas noise policies deal with known, more or less predictable events that do occur with every aircraft operation.” [page 3-11]

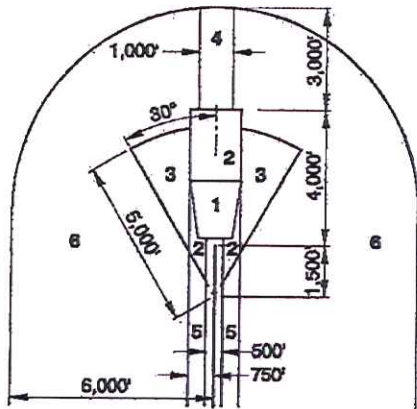
- The boundaries of safety zones are based on the frequency and spatial distribution of aircraft accidents in the vicinity of airports of specific types.
- The compatibility criteria are based on the potential accident consequences, weighed against probabilities of an accident at a given location, and, for some uses, the potential for the severity of the accident to be increased as a result of the character of the land use.



**Example 1:  
Short General Aviation Runway**

**Assumptions:**

- Length less than 4,000 feet
- Approach visibility minimums  $\geq 1$  mile or visual approach only
- Zone 1 = 250' x 450' x 1,000'
- See Note 1.



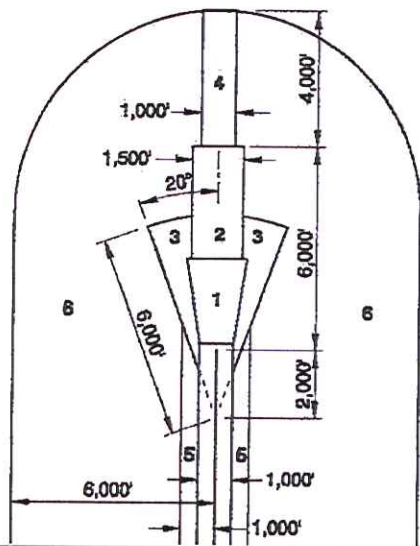
**Example 2:  
Medium General Aviation Runway**

**Assumptions:**

- Length 4,000 to 5,999 feet
- Approach visibility minimums  $\geq 3/4$  mile and  $< 1$  mile
- Zone 1 = 1,000' x 1,510' x 1,700'
- See Note 1.

- ♦ Zone 1: Runway protection zone and within runway object free area adjacent to the runway;
- ♦ Zone 2: Inner approach/departure zone;
- ♦ Zone 3: Inner turning zone;
- ♦ Zone 4: Outer approach/departure zone;
- ♦ Zone 5: Sideline zone; and
- ♦ Zone 6: Traffic pattern zone (not applicable to large air carrier airports).

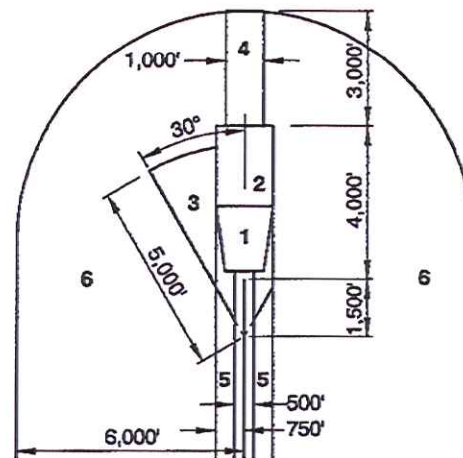




**Example 3:  
Long General Aviation Runway**

**Assumptions:**  
 • Length 6,000 feet or more  
 • Approach visibility minimums < 3/4 mile  
 • Zone 1 = 1,000' x 1,750' x 2,500'  
 See Note 1.

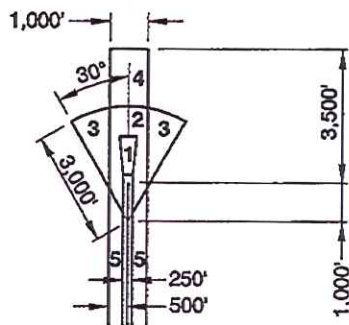
- ◆ *Zone 1:* Runway protection zone and within runway object free area adjacent to the runway;
- ◆ *Zone 2:* Inner approach/departure zone;
- ◆ *Zone 3:* Inner turning zone;
- ◆ *Zone 4:* Outer approach/departure zone;
- ◆ *Zone 5:* Sideline zone; and
- ◆ *Zone 6:* Traffic pattern zone (not applicable to large air carrier airports).



**Example 4:  
General Aviation Runway with  
Single-Sided Traffic Pattern**

**Assumptions:**  
 • No traffic pattern on right  
 • Length 4,000 to 5,999 feet  
 • Approach visibility minimums ≥ 3/4 mile  
 and < 1 mile  
 • Zone 1 = 1,000' x 1,510' x 1,700'  
 See Note 1.



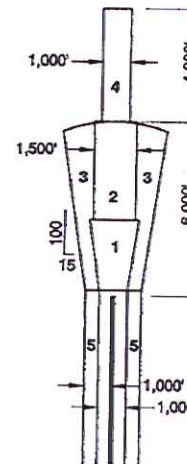


**Example 5:  
Low-Activity General Aviation Runway**

**Assumptions:**

- Less than 2,000 takeoffs and landings per year at individual runway end.
  - Length less than 4,000 feet
  - Approach visibility minimums  $\geq$  1 mile or visual approach only
  - Zone 1 = 250' x 450' x 1,000'
- See Note 1.

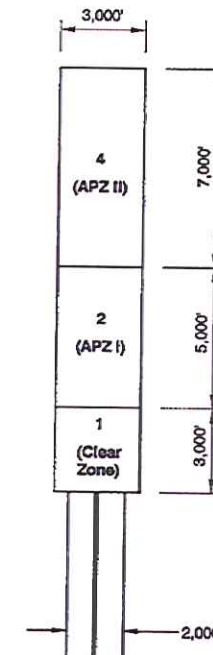
- ◆ Zone 1: Runway protection zone and within runway object free area adjacent to the runway;
- ◆ Zone 2: Inner approach/departure zone;
- ◆ Zone 3: Inner turning zone;
- ◆ Zone 4: Outer approach/departure zone;
- ◆ Zone 5: Sideline zone; and
- ◆ Zone 6: Traffic pattern zone (not applicable to large air carrier airports).



Large Air Carrier Runway

**Assumptions:**

- Minimal light-aircraft general aviation activity
- Predominately straight-in and straight-out flight routes
- Approach visibility minimums  $<$  3/4 mile
- Zone 1 = 1,000' x 1,750' x 2,500'



Military Runway for Large Aircraft

**Assumptions:**

- Military airport
- Predominately straight-in and straight-out flight routes (must modify for turning routes and traffic pattern activity)

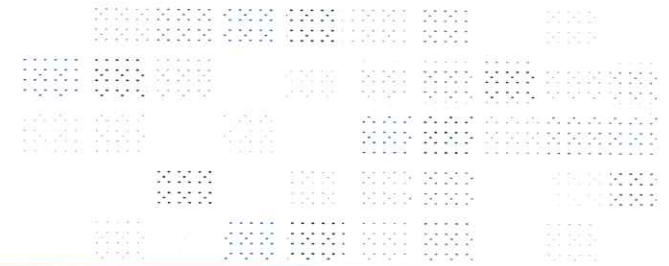
**TABLE 3B: ANALYSIS OF SAFETY ZONE EXAMPLES  
(GENERAL AVIATION RUNWAYS)**

Safety Zone	Example 1: Runway Length Less than 4,000 Feet			Example 2: Runway Length 4,000 to 5,999 Feet			Example 3: Runway Length 6,000 Feet or More		
	% of Points	Acres	% / Acre	% of Points	Acres	% / Acres	% of Points	Acres	% / Acres
<b>All Accident Sites</b>									
Primary Surface	18%	-	-	15%	-	-	13%	-	-
Zone 1: Runway Protection Zone	21%	8	2.65	21%	49	0.40	20%	79	0.26
Zone 2: Inner Approach/Departure Zone	22%	44	0.50	10%	101	0.10	8%	114	0.07
Zone 3: Inner Turning Zone	4%	50	0.08	7%	151	0.05	7%	131	0.05
Zone 4: Outer Approach/Departure Zone	2%	35	0.07	5%	69	0.07	6%	92	0.07
Zone 5: Sideline Zone	5%	-	-	5%	-	-	3%	-	-
Zone 6: Traffic Pattern Zone	18%	-	-	23%	-	-	29%	-	-
Total: Zones 1-6 + Primary Surface	91%	-	-	85%	-	-	85%	-	-

## Variables Indicating Potential Need to Adjust Safety Zones Being Delineated

- Topography
- Displaced Landing Threshold(s)
- Use by Special-Purpose Aircraft
- Non-Precision Approaches
- Circling Approaches
- Special Flight Procedures or Directives
- Predominant Use in One Direction





## Residential Density

**For suburban airports, the Handbook recommends the following density limits:**

- Zone 1: No new dwelling units (DUs)
- Zone 2: 1 DU per 10 to 20 acres
- Zone 3: 1 DU per 2 to 5 acres
- Zone 4: 1 DU per 2 to 5 acres
- Zone 5: 1 DU per 1 to 2 acres
- Zone 6: Not limited by safety;  
consider noise and overflight.

For rural airports, it is recommended that current zoning be maintained if densities are less than the suburban allowances referenced above.

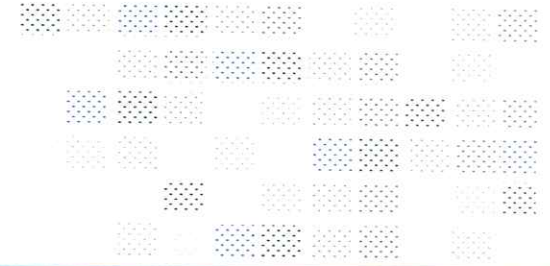
## Land Uses

- Normally Allowable
- Limit
- Avoid
- Prohibit



## AIRPORT LAND USE CONSORTIUM CONFERENCE

2002 Handbook: Rural/Suburban Airport	NONRESIDENTIAL INTENSITIES	2011 Handbook: Suburban Airport	NONRESIDENTIAL INTENSITIES
AVERAGE INTENSITIES	SINGLE-ACRE INTENSITIES	AVERAGE INTENSITIES	SINGLE-ACRE INTENSITIES
Zone 2: 25 - 40	Zone 2: 50 - 80 RRDB + 50%	Zone 2: 40 - 60	Zone 2: 80 - 120
Zone 3: 60 - 80	Zone 3: 120-160 RRDB +100%	Zone 3: 70 - 100	Zone 3: 210 - 300
Zone 4: 60 - 80	Zone 4: 180-240 RRDB +100%	Zone 4: 100 - 150	Zone 4: 300 - 450
Zone 5: 80 - 100	Zone 5: 160-200 RRDB +100%	Zone 5: 70 - 100	Zone 5: 210 - 300
Zone 6: 150	Zone 6: 450 RRDB +100%	Zone 6: 200 - 300	Zone 6: 600 - 800



## Airspace Protection

**“Primary responsibility for preventing hazardous obstructions to airport airspace rests with state and local governments and the airport operator.” [page 3-28]**

- Height and Elevation
- Wildlife Attractant Hazards
- Glint and Glare
- Upward-Directed Lighting
- Smoke or Water Vapor
- Thermal Plumes
- Electrical Interference





## AIRPORT LAND USE CONSORTIUM CONFERENCE

RUNWAY LENGTH > 3,200 FEET	RUNWAY LENGTH UP TO 3,200 FEET	HELIPORT	
SLOPE: 100:1	SLOPE: 50:1	SLOPE: 25:1	
DISTANCE: 20,000 FEET	DISTANCE: 10,000 FEET	DISTANCE: 5,000 FEET	

## Amending the ALUCP

State law limits to once per calendar year. Triggering events may include:

- Change in the Airport Master Plan or Airport Layout Plan affecting aircraft operations
- Change in Handbook Guidelines
- Request from Local Jurisdiction
- Initiation by Commission