



# AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY AGENDA

Riverside County Administration Center  
4080 Lemon Street, 1st Floor Hearing Room  
Riverside, California

Thursday 9:00 A.M., April 9, 2015

**CHAIR**  
Simon Housman  
Rancho Mirage

**VICE CHAIRMAN**  
Rod Ballance  
Riverside

**COMMISSIONERS**

Arthur Butler  
Riverside

Glen Holmes  
Hemet

John Lyon  
Riverside

Greg Pettis  
Cathedral City

Steve Manos  
Lake Elsinore

NOTE: If you wish to speak, please complete a "SPEAKER IDENTIFICATION FORM" and give it to the Secretary. The purpose of the public hearing is to allow interested parties to express their concerns. Comments shall be limited to 5 minutes and to matters relevant to the item under consideration. Please do not repeat information already given. If you have no additional information, but wish to be on record, simply give your name and address and state that you agree with the previous speaker(s). Also please be aware that the indicated staff recommendation shown below may differ from that presented to the Commission during the public hearing.

Non-exempt materials related to an item on this agenda submitted to the Airport Land Use Commission or its staff after distribution of the agenda packet are available for public inspection in the Airport Land Use Commission's office located at 4080 Lemon Street, 14<sup>th</sup> Floor, Riverside, CA 92501 during normal business hours.

In compliance with the Americans with Disabilities Act, if any accommodations are needed, please contact Barbara Santos at (951) 955-5132 or E-mail at [basantos@rctlma.org](mailto:basantos@rctlma.org). Request should be made at least 48 hours or as soon as possible prior to the scheduled meeting.

**STAFF**

Director  
Ed Cooper

John Guerin  
Russell Brady  
Barbara Santos

County Administrative Center  
4080 Lemon St, 14<sup>th</sup> Floor  
Riverside, CA 92501  
(951) 955-5132

[www.rcaluc.org](http://www.rcaluc.org)

## 1.0 INTRODUCTIONS

1.1 CALL TO ORDER

1.2 SALUTE TO FLAG

1.3 ROLL CALL

## 2.0 PUBLIC HEARING: CONTINUED CASE

### **MARCH AIR RESERVE BASE**

2.1 ZAP1107MA14 – Proficiency 215 LLC/Proficiency Capital LLC/Jeff Trenton (Representative: Pam Steele, MIG/Hogle-Ireland) – March Joint Powers Authority (JPA) Case No. PP 14-02 (Plot Plan). A proposal to build a 709,083 square foot industrial warehouse (including 15,000 square feet of office area, 3,000 square feet of which will be on a mezzanine level) on 39.42 acres (Assessor's Parcel Nos. 297-100-013 and 297-100-045) located southerly of Alessandro Boulevard, easterly of Interstate 215, westerly of Old 215 Frontage Road, and northerly of Cactus Avenue. (Airport Compatibility Zones B1-APZ I and B1-APZ II of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan). Continued from February 5, and March 12, 2015. ALUC Staff Planner: John Guerin at (951) 955-0982, or e-mail at [jguerin@rctlma.org](mailto:jguerin@rctlma.org)

Recommendation: CONDITIONAL CONSISTENCY

**3.0 PUBLIC HEARING: NEW CASES****MARCH AIR RESERVE BASE**

- 3.1 ZAP1111MA15 – HHI Riverside, LLC (Representative: Joe Mineo) – County Case Nos. PP25699 (Plot Plan) and VAR 01893 (Variance). The applicant proposes to develop a 19,558 square foot retail and dining center on 3.42 net acres (5.06 gross acres) located at the northeasterly corner of Cajalco Expressway and Harvill Avenue in the unincorporated community of Mead Valley. Plot Plan No. 25699 would authorize development of two retail commercial buildings with a total floor area of 12,872 square feet (one of which would include a 1,440 square foot coffee shop served by a drive-thru lane) and two freestanding restaurants with drive-thru (3,434 and 3,252 square feet, respectively, one of which would be a Farmer Boys). Variance Case No. 1893 is a proposal to exceed the allowable number, height, and surface area of on-site advertising signs. Specifically, the applicant proposes a pylon sign 75 feet in height with a total display area of 540 square feet, in addition to two freestanding monument signs. (Airport Compatibility Zone C2 of the March Air Reserve Base/Inland Port Airport Influence Area). ALUC Staff Planner: Russell Brady at (951) 955-0549, or e-mail at [rbrady@rctlma.org](mailto:rbrady@rctlma.org)

Staff Recommendation: CONDITIONAL CONSISTENCY

**PALM SPRINGS INTERNATIONAL AIRPORT**

- 3.2 ZAP1025PS15 – PS Country Club, LLC (Representative: Marvin Roos, MSA Consulting, Inc.) – City of Palm Springs Case Nos. 5.1327, PD-366, and Tentative Tract Map No. 36691 “Serena Park”. The applicant proposes to develop 429 single-family residential dwellings within the area that was formerly the Palm Springs Country Club golf course. Tentative Tract Map No. 36691 would divide 131.25 acres located northerly of Verona Road, easterly of Sunrise Way, and southwesterly of the Whitewater River Channel into 429 residential lots (292 detached units and 137 attached units), 5.39 acres of public open space, and 25 private open space lots. Case No. 5.1327 is a proposal to amend the General Plan designation of 125.86 acres from Open Space-Parks/Recreation (OS-P/R) to Very Low Density Residential (up to 4.0 dwelling units per acre) (VLDR). PD-366 is a proposal to place the same 125.86 acres in a Planned Development District, superseding the existing O and O5 open space zoning. (Airport Compatibility Zones C and D of the Palm Springs International Airport Influence Area). ALUC Staff Planner: Russell Brady at (951) 955-0549, or e-mail at [rbrady@rctlma.org](mailto:rbrady@rctlma.org)

Staff Recommendation: CONSISTENT(GPA); CONDITIONAL CONSISTENCY (Tract Map)

**FLABOB AIRPORT**

3.3 ZAP1023FL14 – River Springs Charter School (Representative: Dave Black) – City of Jurupa Valley Major Action Case No. MA1492, consisting of Public Use Permit No. 1401. A proposal to relocate and establish Flabob Airport Preparatory Academy as a charter school, including the development of 17,690 square feet of building area (ten classrooms to serve elementary, secondary, and high school students and a 400 square foot administrative office), to be located in the northwesterly portion of the grounds of Flabob Airport, southerly of 42<sup>nd</sup> Street, easterly of Fort Drive and westerly of Twining Street. (Airport Compatibility Zone D of the Flabob Airport Influence Area). ALUC Staff Planner: Russell Brady at (951) 955-0549, or e-mail at [rbrady@rctlma.org](mailto:rbrady@rctlma.org)

Staff Recommendation: INCONSISTENT

**CHINO AIRPORT**

3.4 ZAP1020CH15 – Wal-Mart Stores, Inc. (Representatives: Kathleen Bonesz, Kimley-Horn and Associates; Jonathan Shardlow, Gresham Savage Nolan and Tilden) – City of Eastvale Case Number 12-0051 (Major Development Plan/Conditional Use Permit, General Plan Amendment [GPA], Change of Zone [CZ], Tentative Tract Map). The applicant proposes to develop Eastvale Crossings, a 218,100 square foot shopping center featuring a 192,000 square foot Wal-Mart, on 24.8 gross acres located at the southeast corner of Archibald Avenue and Limonite Avenue, northerly and westerly of Southern California Edison transmission lines and James C. Huber Park. The shopping center will also include two retail buildings with a combined floor area of 18,400 square feet, a 3,500 square foot fast food restaurant with drive-thru, and a gas station with a 4,200 square foot convenience store, 16 fueling positions, and car wash. The site is presently split between areas designated Light Industrial and Commercial Retail on the City’s General Plan. The GPA would designate the entire site as Commercial Retail. Similarly, the site is presently split between areas zoned C-1/C-P (General Commercial) and M-SC (Manufacturing-Service Commercial). The CZ would apply C-1/C-P zoning to the entire site. Finally, Tentative Tract Map No. 35061 would divide the property into six (6) parcels. (Compatibility Zones C and D of the Chino Airport Influence Area). ALUC Staff Planner: Russell Brady at (951) 955-0549, or e-mail at [rbrady@rctlma.org](mailto:rbrady@rctlma.org)

Staff Recommendation: CONSISTENT (with risk reduction design bonus of 20% and limitation on dining area in fast food restaurant)

4.0 **ADMINISTRATIVE ITEMS**

4.1 Director’s Approvals

5.0 **APPROVAL OF MINUTES**

March 12, 2015

6.0 **ORAL COMMUNICATION ON ANY MATTER NOT ON THE AGENDA**

7.0 **COMMISSIONER’S COMMENTS**

**COUNTY OF RIVERSIDE  
AIRPORT LAND USE COMMISSION**

**STAFF REPORT**

**AGENDA ITEM:** 2.1 ~~3.3~~

**HEARING DATE:** *April 9, 2015* ~~March 12, 2015~~ (continued from *March 12 and February 5, 2015*)

**CASE NUMBER:** ZAP1107MA14 – Proficiency 215 LLC/Proficiency Capital LLC/Jeff Trenton (Representative: Pam Steele, MIG/Hogle-Ireland)

**APPROVING JURISDICTION:** March Joint Powers Authority

**JURISDICTION CASE NO:** ~~CZ14-01 (Change of Zone), PP14-02 (Plot Plan), GPA15-01 (General Plan Amendment)~~

**MAJOR ISSUES:** ~~None.~~ *Air Force Reserve Command officials have advised that the basins at this site should be covered due to the proximity to the runway and location directly underlying the extended runway centerline, noting that standing water would be a bird attractant.*

~~*They are recommending a design similar to the approach to water detention being taken at the General Terminal. ALUC staff will be meeting with representatives of the Air Force, Joint Powers Authority staff, and the applicant team on February 26 to try to reach consensus on the approach to minimize wildlife attractants.*~~

*Staff recommends addition of Condition Nos. 11 and 12.*

*Staff has received one e-mail in opposition to the project, specifically in opposition to the location of the point of access off Old 215 Frontage Road.*

*The associated General Plan Amendment and Change of Zone were determined to be consistent with the 2014 March Air Reserve Base Airport Land Use Compatibility Plan on March 12, 2015.*

**RECOMMENDATION:** Staff recommends

~~*that the proposed General Plan Amendment and Change of Zone be found CONSISTENT with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan. Until an appropriately worded condition is added reflecting the character of the mutually acceptable method of maintaining water quality in a manner that does not increase the potential for bird strike, staff*~~ Staff further recommends that consideration of the Plot Plan be CONTINUED to April 9, 2015 to allow for resolution of the Air Force concerns regarding the water retention basins. ~~*Staff is confident that there is a reasonable probability that a consensus will be reached*~~

~~on February 26 or shortly thereafter, enabling staff to recommend~~

~~a finding of CONDITIONAL CONSISTENCY for the Plot Plan, subject to the conditions included herein and such additional conditions as may be necessary to comply with the requirements of the Federal Aviation Administration Obstruction Evaluation Service.~~

~~by the hearing date of March 12, 2015. , subject to the conditions included herein and such additional conditions as may be necessary to comply with FAA requirements.~~

**PROJECT DESCRIPTION:** The applicant proposes to establish Industrial zoning on 39.42 acres and to build a 709,083 square foot industrial warehouse building (including 15,000 square feet of office area, 3,000 square feet of which would be at a mezzanine level) on the property. ~~The project also includes a General Plan Amendment to establish an Industrial General Plan designation on the easterly 6.2 acres of the property, which has just been annexed into the March Joint Powers Authority's land use jurisdiction.~~

**PROJECT LOCATION:** The site is located southerly of Alessandro Boulevard, easterly of Interstate 215, westerly of Old 215 Frontage Road, and northerly of Cactus Avenue within the land use jurisdiction of the March Joint Powers Authority, approximately 5,440 feet northwesterly of the northwesterly terminus of Runway 14-32 at March Air Reserve Base.

**LAND USE PLAN:** 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

- a. Airport Influence Area: March Air Reserve Base
- b. Land Use Policy: Compatibility Zones B1- APZ I and B1 – APZ II
- c. Noise Levels: 65-75 CNEL

**BACKGROUND:**

Non-Residential Land Use Intensity: The site is located in Compatibility Zones B1 - APZ I and B1 – APZ II of the March Air Reserve Base/Inland Port Airport Influence Area. Non-residential intensity is limited to an average of 25 persons per acre within Compatibility Zone B1 – APZ I and an average of 50 persons per acre within Compatibility Zone B1 – APZ II. Single-acre intensities are limited to a maximum of 100 persons within any given acre. (There are no risk-reduction design bonuses available, as March is primarily utilized by large aircraft weighing more than 12,500 pounds.)

Average Intensity

The site is 39.42 acres in area. The site is located in Compatibility Zone B1, and the boundary between Accident Potential Zones I and II (distance of 8,000 feet from the runway terminus) crosses the proposed building location. The more restrictive of these zones limits intensity to a maximum of

25 persons per acre. If the entire site were in APZ I, the allowable total intensity would be 985 persons. The applicant is proposing an industrial warehouse with a total building area of 709,083 square feet, including 15,000 square feet of office space (3,000 square feet of which is in the mezzanine area). The total number of persons that would be expected to be at this facility would be 769 persons if the structure were to be utilized as an e-commerce or fulfillment center, or 561 persons if the structure were to be utilized as a high-cube logistics warehouse. In order to comply with single-acre intensity limitations, the applicant has agreed to limit the warehouse use to that of a high-cube logistics warehouse. On that basis, the average intensity of the project would be 14 persons per acre, which is clearly consistent with the APZ I limitation of 25 persons per acre.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per standard vehicle and 1.0 persons per trailer truck in the absence of more precise data). Based on the number of parking spaces provided (407 standard vehicle spaces and 237 trailer truck loading spaces), the total occupancy would be estimated at 848 people for an average acre intensity of approximately 22, which is also consistent with the APZ I average acre intensity criteria.

#### Single-Acre Intensity

Single-acre intensity in Compatibility Zone B1 is limited to a maximum of 100 persons for areas in either Accident Potential Zone I or Accident Potential Zone II. The most intensely used single acre would be an acre that included 10,000 square feet of office area (7,000 square feet on the ground floor and 3,000 square feet of mezzanine office space), with the remainder of the acre in high-cube warehouse use. Theoretically, the area in warehouse use could be as high as 36,560 square feet within the given acre. Given that the project is proposed as a high-cube logistics warehouse with a floor area of 200,000 square feet or greater, the projected occupancy level is 35 percent of the Building Code maximum for warehouses (one person per 500 times 0.35) and 50 percent of the Building Code maximum for offices (one person per 100 times 0.50), for a single-acre maximum of 76 persons (10,000 divided by 100, divided by 2 = 50 in offices, plus 36,560 divided by 500, times 0.35 = 26 in warehouse area). However, the actual warehouse area within the acre that is proposed to include the 10,000 square feet of office area is less than 36,560 square feet due to the design of that particular portion of the building, such that the single-acre area that includes the office also includes 7,725 square feet outside the building. Thus, the actual warehouse area within that single-acre area is 28,835 square feet. Pursuant to the calculation for high-cube logistics warehouses, this area translates into 20 warehouse employees, for an actual single-acre maximum of 70. This most intensely used single-acre area is in the northerly portion of the building, which is in Accident Potential Zone II.

Staff also checked the most intense single-acre area within Accident Potential Zone I (APZ I). Pursuant to the applicant's agreement to limit office area within APZ I to 5,000 square feet, and given that there is no second floor or mezzanine permissible in APZ I, the projected occupancy for the most intense single-acre area within APZ I would be (5,000 divided by 100, divided by 2 = 25 in offices, plus 38,560 divided by 500, times 0.35 = 27 in warehouse area), for a single-acre maximum

of 52.

Site Design/On-Site Locational Criteria: Within Airport Compatibility Zone B1, criteria specify that structures are to be located a “maximum distance from the extended runway centerline.” The extended runway centerline passes directly over the easterly portion of this property. The project design is generally in compliance with this criterion. The exception is that the design provides for automobile parking along the westerly side of the property, which is the area farthest from the extended runway centerline. However, this may be the only location where such parking is feasible. The easterly side of the building has been designed to provide for truck docking, which renders use of that area for automobile parking infeasible. The applicant has been careful to design the project so that the structure does not straddle or approach the location of the extended runway centerline. The underlying area is used primarily for trailer parking. Furthermore, when trucks are not in the docked position, there is an extensive open area directly easterly of the building that would potentially be available in the event of a controlled landing.

Lot coverage within Accident Potential Zones is limited to a maximum of 50 percent. Using a conservative approach that does not include land within the adjacent surface street rights-of-way, the proposed project has a lot coverage of 41.49 percent. Considering the two APZs separately, lot coverage is 42.57 percent in APZ I and 38.87 percent in APZ II. As the lot coverage in APZ I exceeds 20 percent, provision of on-site services to the public in the portion of the site within APZ I is prohibited.

The number of aboveground habitable floors is limited to one story in APZ I and two stories in APZ II. The proposed building complies with these limits. The building is one story, with the exception of a mezzanine that is limited to 3,000 square feet within APZ II.

Zoned fire sprinkler systems are required.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zone B1 within the project.

**Hazards to flight are, of course, prohibited in Airport Compatibility Zone B1. Air Force Reserve Command officials have raised concerns that uncovered water in the detention basins would be a bird attractant and have suggested that water quality and drainage needs be addressed in a manner similar to the approach taken by March Inland Port Airport Authority at its new general aviation terminal. Staff is hoping that these concerns can be resolved through a mutually acceptable solution formulated at, or shortly after, the meeting scheduled for February 26.**

*Staff has been working with the applicant team and Air Force representatives to formulate special conditions to address the issues raised by the Air Force Reserve Command. See Conditions 11 and 12 below.*

Noise: The entire site is located within the 65 CNEL contour from operations associated with

aircraft departing from and/or landing at March Air Reserve Base/Inland Port Airport, and the southerly portion of the site is located within the 70 CNEL contour. Thus, this site is one of the most heavily impacted by aircraft noise among all off-Base locations. At these anticipated exterior noise levels, special measures would be required to mitigate aircraft-generated noise within the office portions of the building so as to achieve an interior noise level of 45 CNEL.

Part 77: The elevation of Runway 14-32 at its northerly terminus is approximately 1535.1 feet above mean sea level (1535.1 feet AMSL). At a distance of approximately 5,920 feet from the runway, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1594.3 feet AMSL. The apparent finished floor elevation of the building is approximately 1541 feet AMSL. The proposed building has a maximum height of 44.3 feet for a potential maximum elevation of 1585.3 feet AMSL. Therefore, review by the FAA Obstruction Evaluation Service would not normally be required. However, March Joint Powers Authority, the jurisdiction of record, requires submittal of Form 7460-1 for all building projects within their area. The applicant has submitted Form 7460-1, and the FAA has assigned Aeronautical Study No. 2015-AWP-566-OE a “Work in Progress” status.

Avigation Easement: Pursuant to Table MA-2 of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, avigation easement dedication (to the March Inland Port Airport Authority) is required for land uses located within Airport Compatibility Zone B1.

**Jurisdictional Matters:** In the course of project review, March Joint Powers Authority (March JPA) officials determined that the easterly 6.2 acres of the project site (Assessor’s Parcel Number 297-100-045, the linear parcel along the easterly portion of the project site) was not located within March JPA’s jurisdiction and was actually still within unincorporated Riverside County. Land use authority over this parcel has since been transferred from the County of Riverside to March JPA through an amendment to the Joint Powers Agreement approved by the Board of Supervisors on February 10, 2015. Under County jurisdiction, this linear parcel was zoned R-R (Rural Residential), the lineal descendant of the County’s original M-3 zoning. The parcel was not given a Riverside County Integrated Project General Plan or Area Plan land use map designation, as it was apparently mistaken for a right-of-way by the County’s Principal Planner John Guerin and General Plan consultants during the formulation of Area Plan land use maps just after the turn of the century.

Therefore, in addition to establishing Industrial zoning on the entire 39.42-acre site, it would be necessary to establish a March JPA General Plan land use designation of Industrial on Assessor’s Parcel Number 297-100-045 and, to that end, the applicant team filed GPA 15-01 with March JPA. Staff has re-advertised this project to reflect the addition of the General Plan Amendment to the project description.

**Other:** Staff has received one letter in opposition to the proposed project, specifically to the location of the access point off Highway 215 Frontage Road on the easterly side of the property. Ground access is not within the realm of ALUC, so staff forwarded copies to the



**March Joint Powers Authority, the City of Moreno Valley, and the project team. A copy is also attached to this staff report.**

**CONDITIONS:**

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, congregate care facilities, hotels/motels, places of assembly, restaurants, hazardous materials manufacture/storage (excluding storage of quantities of less than 6,000 gallons of flammable materials in the APZ II portion of the property), noise sensitive outdoor nonresidential uses, and hazards to flight.
  - (f) Retail trade, eating and drinking establishments, personal services, professional services, educational services, governmental services, medical facilities, cultural activities, and any other uses providing on-site services to the public.
  - (g) Commercial/service uses; civic uses; churches, chapels, and other places of worship; classrooms; gymnasiums; theaters; conference or convention halls; auditoriums;

fraternal lodges; bowling alleys; gaming; auction rooms.

- (h) Manufacturing of: food and kindred products, textile mill products, apparel, chemicals and allied products, rubber and plastic products, fabricated metal products, professional, scientific, and controlling instruments, photographic and optical goods, watches and clocks.
3. Prior to issuance of any building permits, the landowner shall convey and have recorded an avigation easement to the March Inland Port Airport Authority. Contact March Joint Powers Authority at (951) 656-7000 for additional information.
4. The attached notice shall be given to all prospective purchasers of the property and/or tenants of the building. While not required, the applicant and its successors-in-interest are encouraged to provide a copy of said notice to employees who would regularly be working at this location.
5. The proposed detention basins on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the retention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the retention basin(s) shall not include trees that produce seeds, fruits, or berries.
6. This project has been evaluated as a proposal for the establishment of a high-cube logistics warehouse with a maximum of 10,000 square feet of office space in the northerly portion of the building and a maximum of 5,000 square feet of office space in the southerly portion of the building. March Joint Powers Authority shall require additional review by the Airport Land Use Commission prior to the establishment of office uses exceeding the amounts specified above.
7. Mezzanine areas shall be limited to a maximum of 3,000 square feet, and shall be permitted only in the northerly portion of the building outside Accident Potential Zone I.
8. Zoned fire sprinkler systems shall be required throughout the building.
9. Office space must have sound attenuation features sufficient to reduce interior noise levels from exterior aviation-related sources to no more than CNEL 45 dB. March Joint Powers Authority shall require an acoustical study to ensure compliance with this requirement.
10. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio

communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.

11. *In order to ensure proper functioning of the project drain system to avoid potential hazards to March Air Reserve Base flights, an additional Best Management Practice (BMP) shall be added to the project Water Quality Management Plan (WQMP), which shall be recorded with the land. The BMP shall include the following program:*
  - a. *The property owner (Proficiency 215 LLC or its successor(s)-in-interest, hereinafter "Owner") or its designated representative shall monitor the conditions of the detention basins and promptly inspect such basins following the completion of each "significant" rain event either within 48 hours following the completion of the rain event or as soon as possible thereafter.*
  - b. *If any standing water remains in a basin that is not beneath a rock, gravel, or other layer following the completion of the "significant" rain event and the 48 hour period thereafter, Owner or its designated representative shall arrange to have such standing water either removed or covered within 24 hours following the conclusion of the 48 hour period.*
  - c. *In the event that the standing water situation recurs on a regular basis beyond the 48-hour detention period, the detention basin may no longer be draining as originally designed to prevent standing water from rising above a rock, gravel or other layer (for example, due to a rise in groundwater levels or other circumstance beyond Owner's ability to control). In that situation, Owner or its designated representative shall promptly engage a licensed civil engineer to prepare a design plan to assure that such condition does not persist for more than 48 hours following the conclusion of a rain event. The required engineered design solution shall be implemented within 90 days of its approval by all applicable authorities. Until such time as the engineered design solution is implemented, Owner or its designated representative shall be responsible for removing or covering any standing water rising above a rock, gravel, or other layer on a daily basis.*
12. *Proficiency 215 LLC (or its successor-in-interest)(hereinafter referred to as "Owner") shall inspect the basins after each significant rainfall event. Owner shall invite March Joint Powers Authority to jointly inspect the basins with the Owner. In the event that standing water is observed in a basin beyond the 48-hour detention period identified in Condition No. 5 above by any party (including aircraft in flight), upon notification to either the United States Air Force or the March Inland Port Airport Authority (the "airport operators"), the airport operators shall notify the Owner in writing. The Owner shall be required to take all measures necessary as soon as possible following receipt of such notification (normally within 24 hours) to either drain or cover the standing water. Should the Owner fail to take such action, Owner authorizes the airport operators to take*

*such action as may be necessary to eliminate a hazard to flight. The Owner shall work with the airport operators to prevent recurrence of standing water situations beyond the 48-hour detention period. For each such incidence made known to the Owner, the necessary remediation shall only be considered to have been fulfilled when the airport operators state in writing that the situation has been remediated to the airport owners' satisfaction. In the event that the standing water situation recurs on a regular basis beyond the 48-hour detention period, the airport operators may require the Owner to implement an engineered design solution pursuant to Condition 11.c. above.*

## Guerin, John

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**From:** Pam Steele <pams@migcom.com>  
**Sent:** Wednesday, March 25, 2015 12:06 PM  
**To:** Guerin, John  
**Cc:** Jeffrey Trenton  
**Subject:** Re: Freeway Business Center - Proficiency 215 LLC  
**Attachments:** Draft Conditions 11 and 12 - Applicant comments 3-25-15.docx

John,  
I was able to discuss the conditions with Jeff - attached are our requested revisions.  
Thank you!

Pam

### Pam Steele

*Principal*

**MIG | Hogle Ireland**  
1500 Iowa Avenue, Suite #110  
Riverside, California 92507  
O: 951 787 9222 | C: 951 733 5240 | [www.migcom.com](http://www.migcom.com)

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On Wed, Mar 25, 2015 at 10:16 AM, Guerin, John <[JGUERIN@rctlma.org](mailto:JGUERIN@rctlma.org)> wrote:

Ed will be reviewing my staff report today. It is possible that staff reports for distribution to Commissioners will be finalized this week, as they will be mailed out to Commissioners Monday or Tuesday. If need be, recommended conditions may be amended later; however, such amendments would require specific discussion at the hearing through Power Point or handouts.

I have incorporated the correction to the name of owner. Thank you.

**From:** Pam Steele [mailto:[pams@migcom.com](mailto:pams@migcom.com)]  
**Sent:** Wednesday, March 25, 2015 10:00 AM  
**To:** Guerin, John  
**Subject:** Fwd: Freeway Business Center - Proficiency 215 LLC

Requested revisions:

11. In order to ensure proper functioning of the project drain system to avoid potential hazards to March Air Reserve Base flights, an additional Best Management Practice (BMP) shall be added to the project Water Quality Management Plan (WQMP), which shall be recorded with the land. The BMP shall include the following program:

- a. The property owner (Proficiency Capital 215 LLC or its successor(s)-in-interest, hereinafter "Owner") or its designated representative shall monitor the conditions of the detention basins and promptly inspect such basins following the completion of each "significant" rain event ~~and the either within~~ 48 hours period following the completion of the rain event or as soon as possible thereafter.
- b. If any standing water remains in a basin that is not beneath a rock, gravel, or other layer following the completion of the "significant" rain event and the 48 hour period thereafter, Owner or its designated representative shall arrange to have such standing water either removed or covered ~~within 24 hours~~ promptly following the conclusion of the 48 hour period. (Please note – we request "promptly" because it could be a weekend or other circumstance that would prevent strict adherence to the 24 hour period.)
- c. In the event that the standing water situation recurs on a regular basis ~~beyond~~ following the 48-hour detention period, the detention basin may no longer be draining as originally designed to prevent standing water from rising above a rock, gravel or other layer (for example, due to a rise in groundwater levels or other circumstance beyond Owner's ability to control). In that situation, Owner or its designated representative shall promptly engage a licensed civil engineer to prepare a design plan to assure that such condition does not persist for more than 48 hours following the conclusion of a rain event. The required engineered design solution shall be implemented ~~within 90 days of~~ promptly following\* its approval by all applicable authorities. Until such time as the engineered design solution is implemented, Owner or its designated representative shall be responsible for removing or covering any standing water rising above a rock, gravel, or other layer ~~on a daily basis~~.

\*The engineered solution COULD take longer than 90 days, therefore, we request "promptly". The sentence following that one addresses continued diligence until the solution is constructed.

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John – As revised, Condition #11 more than covers the issue. Jeff Trenton noted that he cannot accept Condition #12. The Air Force sold the property for development and has delegated authority to March JPA for this type of compliance review and can work through that body to ensure compliance.

12. ~~Proficiency Capital 215 LLC (or its successor-in-interest)(hereinafter referred to as "Owner") shall inspect the basins after each 48 hour period following a significant rainfall event. Owner shall invite March Joint Powers Authority shall have the right to jointly inspect the basins with the Owner. In the event that standing water rising above a rock, gravel or other layer is observed in a basin beyond the 48-hour detention period identified in Condition No. 5 11 above by any party (including aircraft in flight), upon notification to either the United States Air Force or the March Inland Port Airport Authority (the "airport operators"), the airport~~

~~operators shall notify the Owner in writing. The Owner shall be required to take all measures necessary as soon as possible following receipt of such notification (normally within 24 hours) to either drain or cover the standing water. Should the Owner fail to take such action, Owner authorizes the airport operators to take such action as may be necessary to eliminate a hazard to flight. The Owner shall work with the airport operators to prevent recurrence of standing water situations beyond the 48-hour detention period. For each such incidence made known to the Owner, the necessary remediation shall only be considered to have been fulfilled when the airport operators state in writing that the situation has been remediated to the airport owners' satisfaction. In the event that the standing water situation recurs on a regular basis beyond the 48-hour detention period, the airport operators may require the Owner to implement an engineered design solution pursuant to Condition 11.c. above.~~



DEPARTMENT OF THE AIR FORCE  
AIR FORCE RESERVE COMMAND

10 March 2015

MEMORANDUM FOR RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

ATTN: JOHN GUERIN  
PRINCIPAL PLANNER  
4080 LEMON STREET, 14<sup>TH</sup> FLOOR  
RIVERSIDE, CA 92501

FROM: 452d Mission Support Group/Civil Engineers  
Base Operating Support  
610 Meyer Drive, Bldg. 2403  
March ARB CA 92518-2166

SUBJECT: Riverside County Airport Land Use Commission (RCALUC) – ZAP1107MA14

1. The March Air Reserve Base (MARB) review of the proposal to establish Industrial zoning on 39.42 acres and to build a 709,083 square foot industrial warehouse building is provided with this memorandum.
2. The parcel also known as D3 East is located within the Accident Potential Zone I (APZ I) and Accident Potential Zone II (APZII). Any construction in APZ I is to consist of facilities that are no greater than single floor, airspace review is required for objects greater than 35ft. in height. Lot coverage is based on calculation in the Floor Area Ratios (FAR). Only a few types of facilities are compatible in this zone. There are restrictions on land uses and heights of natural objects and man-made objects in the vicinity of air installations that may obstruct the airspace, attract birds, cause electromagnetic or thermal interference, or produce dust, steam, smoke, or light emissions to provide for safety of flight and the public welfare.
3. The parcel is partially located within the Perris North sub-basin, the same groundwater sub-basin as MARB. The rising groundwater table at MARB is an ongoing concern and solutions are being sought by both MARB and the State of California. Given the concerns with the rising groundwater and the ability to properly drain the water detention basins within 48 hours, MARB is requesting the water detention basins be oversized enough to accept additional rock to address future concerns with groundwater or be underground and covered. Prior to issuance of formal approval, we want to see specific design of basins and if uncovered, calculations that show capacity if rock is added later.
4. A properly designed stormwater management system and landscaping must address Bird/Wildlife Aircraft Strike Hazard (BASH) concerns including proper detention/infiltration of stormwater runoff. The base will want to review details of the stormwater conveyance system and the landscaping plan when they become available. Given the proximity to the airfield, trees which will bear mast or grow to an adequate size for roosting should not be planted. Additional information on reducing BASH hazards can be found in AFPAM 91-212, *Bird/Wildlife Aircraft Strike Hazard (BASH) Management Techniques*, dated February 1, 2004. We request that March Joint Powers Authority evaluate the stormwater detention basin design to mitigate or eliminate any hazards, and jointly approve the design with MARB.
5. The property is impacted by aircraft noise with California Noise Equivalency Levels and a Day-Night Average A-Weighted Sound Level (DNL) of 65 decibels or more contributing to negatively impact



compatible and reasonable use of the property. Table 2, *Land Use Compatibility in Noise Zones* in Appendix 3 to Enclosure 3, *Recommended Land Use Compatibility in Noise Zones* of DoDI 4165.57, *Air Installations Compatible Use Zones* (AICUZ) states that noise events may be sufficient to periodically disrupt indoor activities. Employees and regularly received public may require protection using noise attenuation in the design and construction of the facility. Additional hearing protection for employees may be required by OSHA or other agencies as it relates to safety and health in a high noise level work environment.

6. While the proposed use may be consistent with the zoning and land use guidelines, MARB advises that the proposed project presents a concern being located so close to the Clear Zone (CZ) at the north end of the runway. Buildings in this area should not be used for high-density functions since the objective of the land use guidelines in and around APZ's is to restrict people-intensive use due to a greater risk of aircraft incident in these areas. In APZ I is restricted to 25 people per acre and 50 an acre in APZ II. Lot coverage is based on the FAR, and is calculated using standard parking generation rates for various land uses, vehicle occupancy rates, and desired density in APZ I and II. For APZ I, the formula is  $FAR = 25 \text{ people an acre} / (\text{Average Vehicle Occupancy} \times \text{Average Parking Rate} \times (43560/1000))$ . The formula for APZ II is  $FAR = 50 / (\text{Average Vehicle Occupancy} \times \text{Average Parking Rate} \times (43560/1000))$ .

7. The building height is a cause for concern. We request the latitude and longitude of the southeast and northeast corners and the ground elevation of the warehouse along this façade in order to confirm the building falls within established height restrictions. Consultation with the Federal Aviation Administration will be required and we will also need to provide a Terminal Instrument Procedures (TERPS) review.

8. To help eliminate any potential effects on aircraft operations at MARB, we ask that materials provided in construction be of a non-reflective material such as outside ductwork, windows and roofs by means such as painting or covering. In addition, none of the project improvements shall create:

- Distracting lights which could be mistaken for airport lights
- Sources of dust, steam, or smoke which may impair pilot visibility
- Sources of electronic interference with aircraft communications or navigation

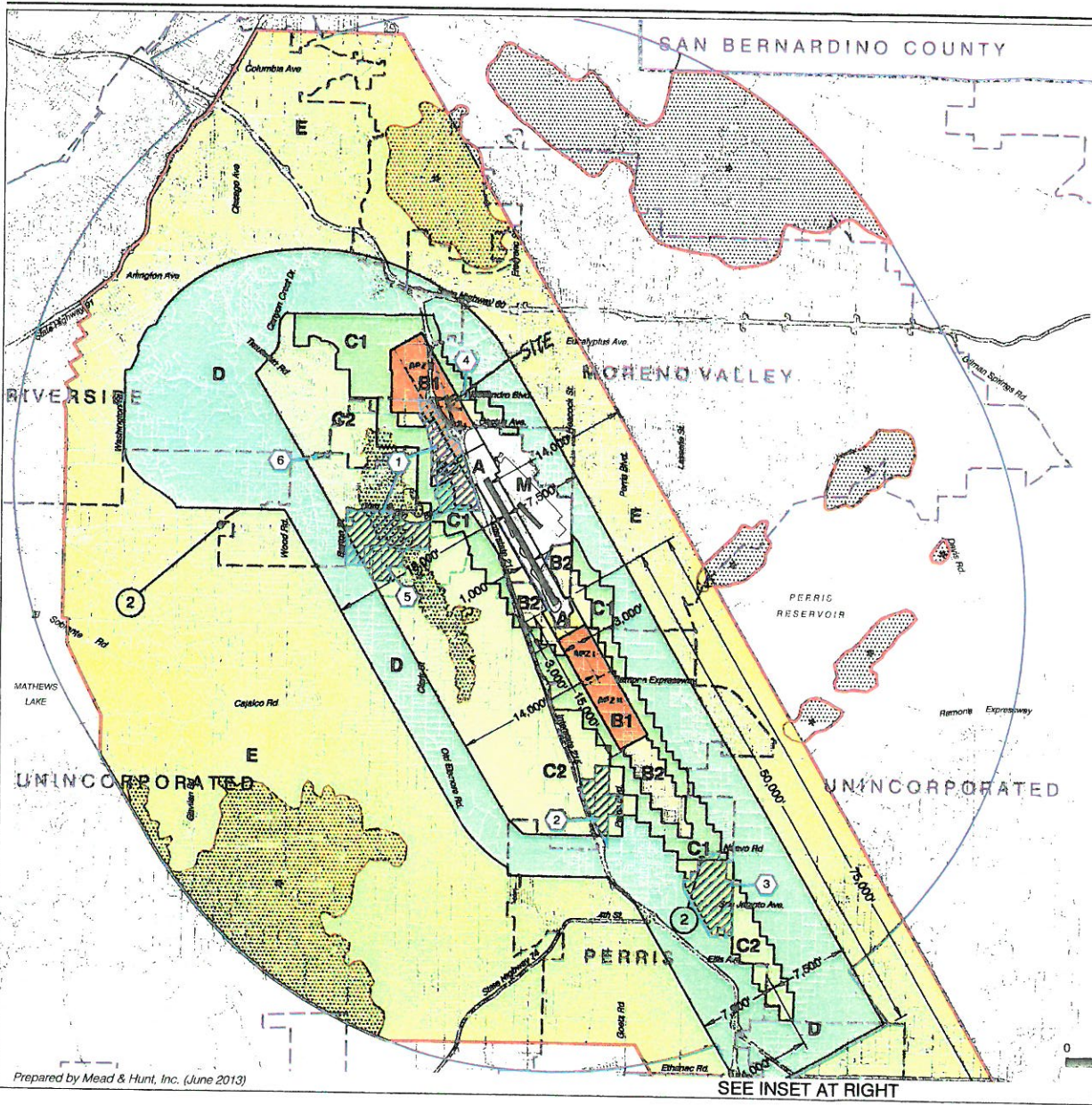
9. Thank you for the opportunity to again, review and comment on this proposed development. If you have questions please contact Ms. Denise Hauser at (951) 655-4862, or Sonia Pierce at (951) 655-2236.



PAMELA M. HANN  
Base Civil Engineer

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



**LEGEND**

**Compatibility Zones**

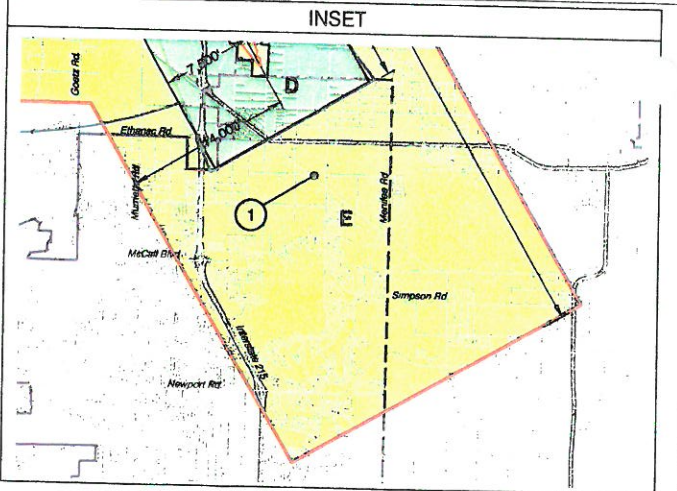
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

**Boundary Lines**

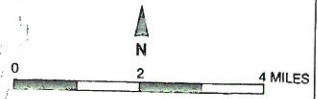
- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



Note:  
All dimensions are measured from runway ends and centerlines.



Base map source: County of Riverside 2013

**Riverside County  
Airport Land Use Commission  
March Air Reserve Base / Inland Port Airport  
Land Use Compatibility Plan  
(Adopted November 13, 2014)**

Map MA-1

**Compatibility Map  
March Air Reserve Base / Inland Port Airport**

SEE INSET AT RIGHT

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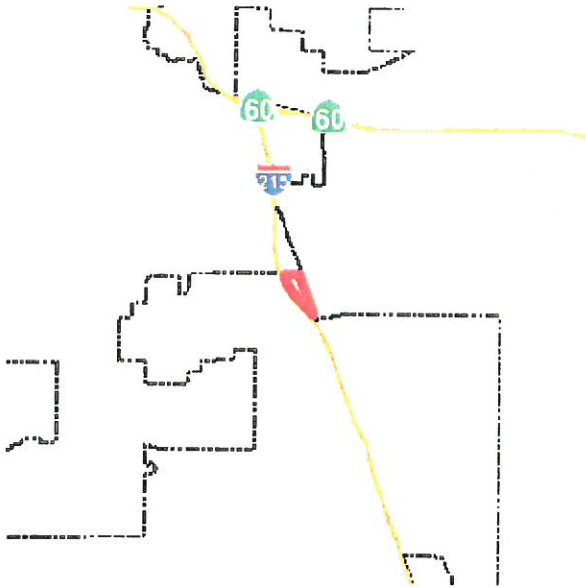
Prepared by Mead & Hunt, Inc. (June 2013)



**Riverside County Parcel Report**  
**APN 297-100-013**  
Disclaimer

Report Date: Tuesday, January 20, 2015

**MAPS/IMAGES**

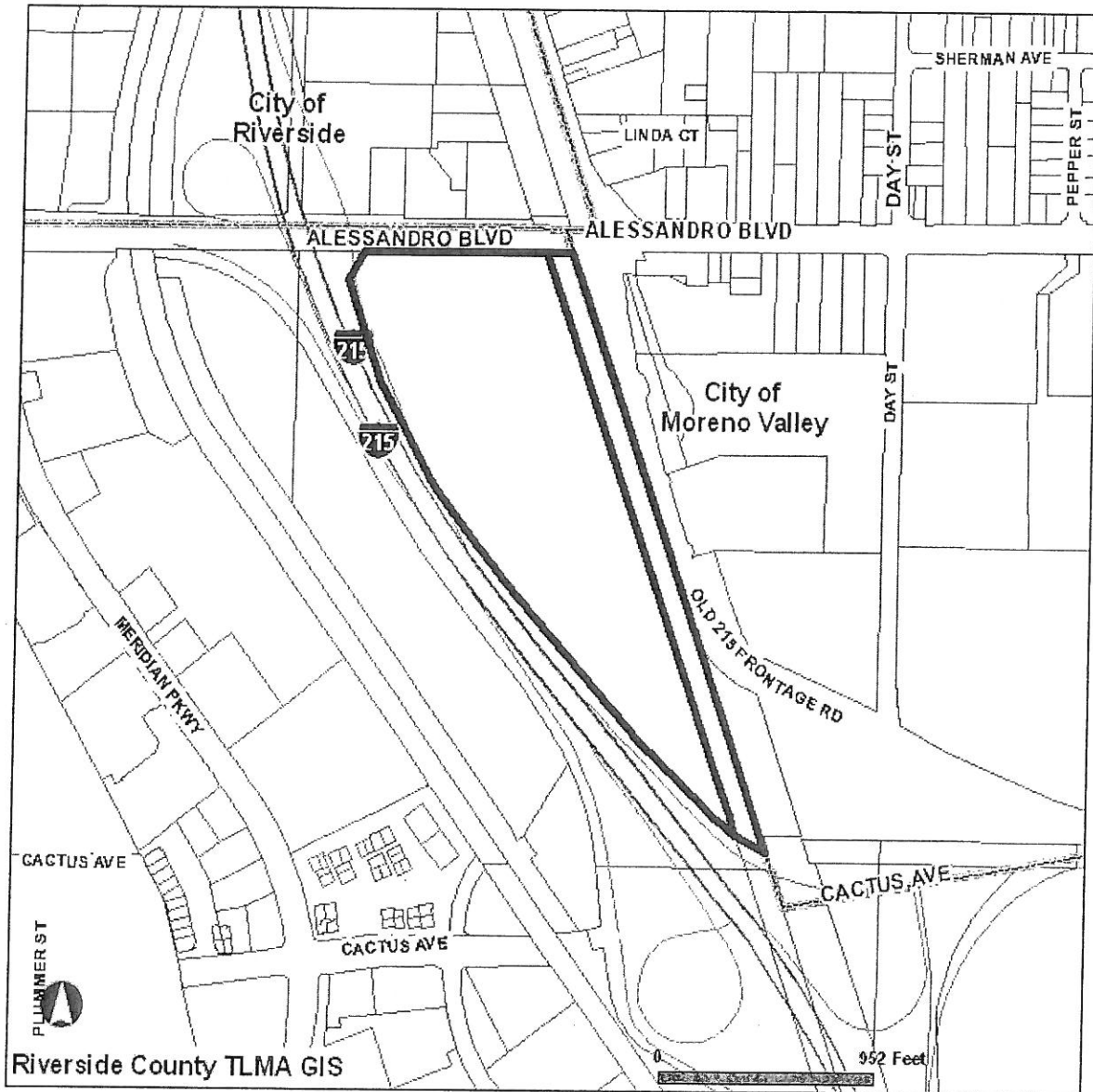


**PARCEL**

<b>APN</b>	<u>297-100-013-9</u>	<b>Supervisory District 2011</b>	KEVIN JEFFRIES, DISTRICT 1
		<b>Supervisory District 2001</b>	BOB BUSTER, DISTRICT 1
<b>Previous APN</b>	297100011	<b>Township/Range</b>	T3SR4W SEC 15
<b>Owner Name</b>	PROFICIENCY 215	<b>Elevation Range</b>	1,524 - 1,548
<b>Address</b>	No address available	<b>Thomas Bros. Map Page/Grid</b>	PAGE: 716 GRID: J6 PAGE: 716 GRID: J7 PAGE: 717 GRID: A6 PAGE: 717 GRID: A7
<b>Mailing Address</b>	C/O PROFICIENCY CAPITAL 11777 SAN VICENTE STE 780 LOS ANGELES CA, CA 90049	<b>Indian Tribal Land</b>	Not in Tribal Land
<b>Legal Description</b>	Recorded Book/Page: <u>MB 6/13</u> Subdivision Name: ALESSANDRO TR Lot/Parcel: 4 Block: 12 Tract Number: Not Available	<b>City Boundary/Sphere</b>	Not within a City Boundary Not within a City Sphere Annexation Date: Not Applicable No LAFCO Case # Available Proposals: Not Applicable

**Lot Size**

RIVERSIDE COUNTY GIS



Selected parcel(s):  
297-100-013 297-100-045

LEGEND

- SELECTED PARCEL
- CITY
- INTERSTATES
- HIGHWAYS
- PARCELS

**\*IMPORTANT\***

Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

297-100-045  
ADDRESS NOT AVAILABLE  
STANDARD WITH PERMITS REPORT

**APNs**

297-100-013-9  
297-100-045-8

**OWNER NAME**

**STANDARD JPA NOTES**

1. ALL NEW OR EXISTING UTILITY LINES SHALL BE 60" OR GREATER IN DIAMETER UNLESS OTHERWISE SPECIFIED.
2. ALL EXISTING UTILITY LINES SHALL BE 60" OR GREATER IN DIAMETER UNLESS OTHERWISE SPECIFIED.
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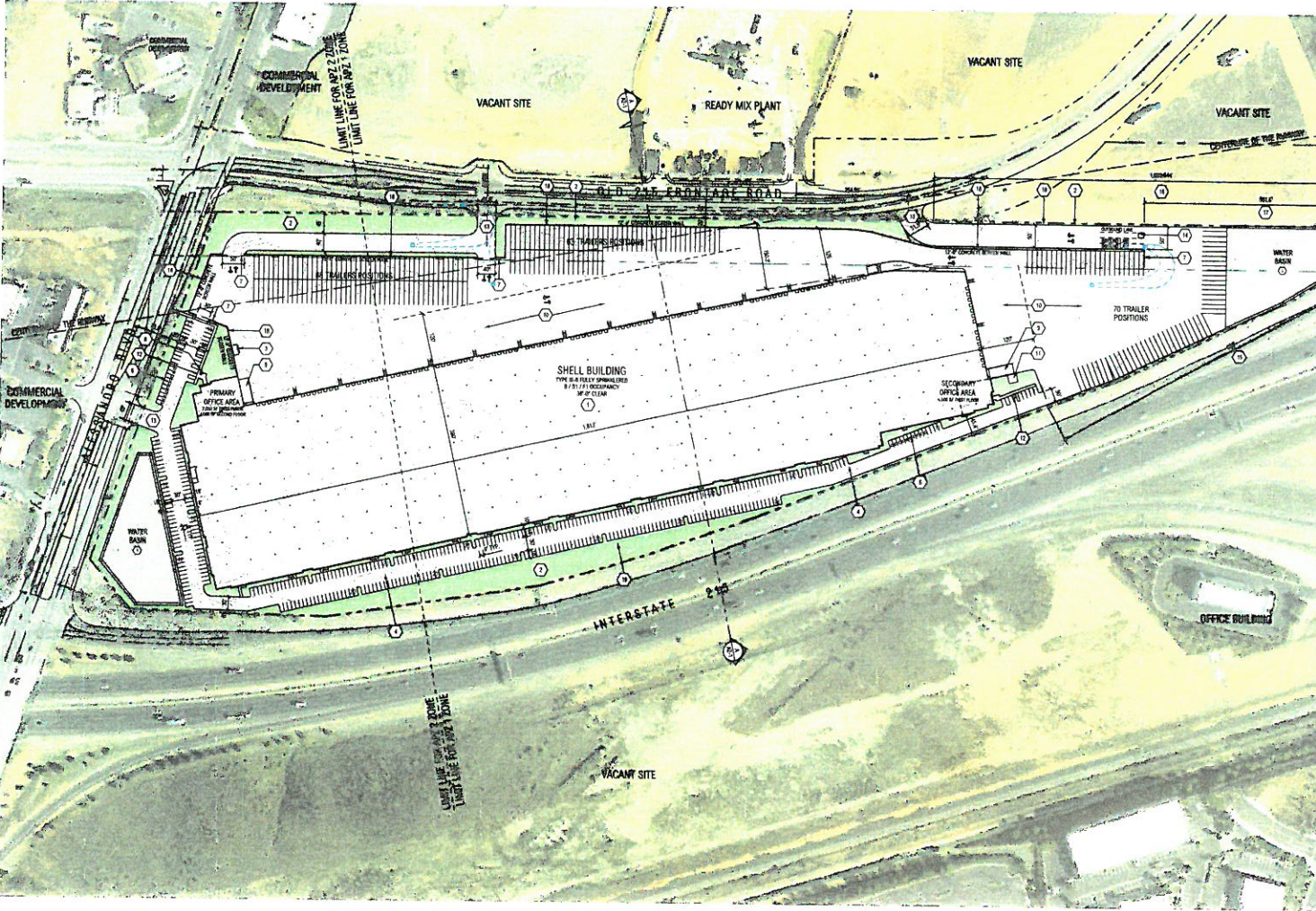
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**LEGAL DESCRIPTION**

THE LAND REFERRED TO HEREIN IS LOCATED IN THE COUNTY OF INVERNO, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS:  
 PARCEL A, MAP: 201-100-04-1  
 THAT PORTION OF SECTION 15, TOWNSHIP 3 SOUTH, RANGE 4 WEST, SAN BERNARDINO COUNTY, IN THE COUNTY OF INVERNO, STATE OF CALIFORNIA, ACCORDING TO THE OFFICIAL PLAT THEREOF, CONVEYED TO THE CALIFORNIA SOUTHWESTERN RAILROAD COMPANY BY DEED RECORDED MAP 23, 1906, IN BOOK 15 PAGE 11 OF BOOKS IN THE OFFICE OF THE COUNTY RECORDER OF SAN BERNARDINO COUNTY, CALIFORNIA.  
 PARCEL B, MAP: 201-100-04-1  
 PARCEL C, MAP: 201-100-04-1  
 PARCEL D, MAP: 201-100-04-1

**PROJECT DATA**

SITE AREA:	1,709,058 SF
BUILDING AREA:	39,230 AC
OVERALL BUILDING FOOTPRINT:	706,083 SF
PRIMARY FIRST FLR OFFICE (NE CORNER):	7,000 SF
PRIMARY SECOND FLR OFFICE (NE CORNER):	3,000 SF
SECONDARY FIRST FLR OFFICE (SW CORNER):	6,000 SF
WAREHOUSE AREA:	684,083 SF
TOTAL:	706,083 SF
COVERAGE:	41.48 %
PARKING REQUIRED:	50 STALLS
0 - 50,000 SF @ 0.33/1000	50 STALLS
50,000 - 200,000 SF @ 0.33/1000	50 STALLS
200,000 SF + @ 0.20/1000	99 STALLS
TOTAL STALLS REQUIRED:	249 STALLS
AUTO PARKING PROVIDED:	386 STALLS
STANDARD STALLS	21 STALLS
CARPOOL STALLS (50% MIN)	21 STALLS
TOTAL STALLS PROVIDED:	407 STALLS
BIKE PARKING:	12 SPACES
REQUIRED @ 1/20 AUTO STALLS	12 SPACES
PROVIDED:	12 SPACES
LOADING DOCK POSITIONS:	109 DOCKS
TRUCK TRAILER STALLS 12' X 52'	237 STALLS
LANDSCAPE AREA PROVIDED:	152,328 SF
11.25 %	
MINIMUM SETBACKS REQUIRED:	
FRONT BUILDING	20'
FRONT LANDSCAPE	0'
SIDE BUILDING	0'
SIDE LANDSCAPE	25'
REAR BUILDING	5'
ROOF OVERHANGING	



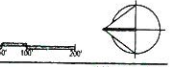
**KEYNOTES**

1. PAINTED CONCRETE IN-TOP WAREHOUSE / OFFICE / MANUFACTURING FACILITY.
2. SHADY AREA: PROPOSED BRICK PAVING WITH STONE CURB AND METAL CURB. CONCRETE CURB AT ALL INTERSECTIONS.
3. PAINTED CONCRETE TRUCK AND TRAILER BAY ENCLOSURE LINE. 4" HIGH CURB ON INTERSECTIONS FOR TRUCK AND TRAILER BAY ENCLOSURE. TRUCK AND TRAILER BAY ENCLOSURE ARE WITHIN THE DESIGNATED TRUCK DOCK.
4. TYPICAL STANDARD PARKING STALL. MIN. 9' X 18' OR 10' X 20'. OVERHANGS - STOPS PER STANDARD.
5. WATER QUALITY BASIN. SEE CIVIL DRAWINGS.
6. OTHER MATERIALS OF STALLS AT CARPOOL STALLS.
7. 12" OF PAINTED CONCRETE TRUCK AND TRAILER BAY ENCLOSURE PER DESIGN NETWORK AND IF CONCRETE CURB AT ALL INTERSECTIONS.
8. ACCESSIBLE: BUS BAY ENTRY WITH ADJACENT BICYCLE RACKS NOT ENCROACHING INTO TRUCK AND TRAILER BAY.
9. CONCRETE PAVED TRUCK BAY.
10. CONCRETE PAVED TRUCK BAY.
11. CONCRETE PAVED TRUCK BAY.
12. ON SITE ACCESSIBLE DRIVEWAY AND CURB RAMP. SLOPE TO BE 2" PER 1' IN FRONT OF AUTO STALLS AND 4" PER 1' OTHERWISE.
13. NEW CURB ON TOP CHANGING.
14. FUTURE GROUND SIGNAGE TO MATCH EXISTING.
15. 4" TUBULAR STEEL POCKET FENCE WITH STONE CLAD CHAIN PLASTERS AT 80% MAX.
16. EXISTING FENCING ON ADJACENT PROPERTY.
17. 4" TUBULAR STEEL POCKET FENCE ALONG INTERIOR PROPERTY LINE. CROSSING: 4" MAX.
18. 12" OF PAINTED CONCRETE SCREEN WALL WITH STONE CLAD CHAIN PLASTERS AT 80% MAX.
19. LIGHTING ALONG INTERIOR PROPERTY LINE AND EXTERIOR PROPERTY LINE SHALL BE DESIGNED TO PROVIDE LIGHT SPILLAGE ONTO ADJACENT PROPERTIES.

**VICINITY MAP**



**SITE PLAN**



**UTILITY PROVIDERS**

- ELECTRIC: SDG&P
- WATER/GAS: WESTERN MUNICIPAL WATER DISTRICT
- TELEPHONE: VERIZON
- GAS: SOUTHERN CALIFORNIA GAS

**REGA**  
 Office of Architectural Design  
 11777 Alessandro Blvd, Suite 700  
 Los Angeles, CA 90049  
 P: 310-979-8000  
 F: 310-979-1772

CONSULTANT  
 PROFESSIONAL SEAL

**FREEWAY BUSINESS CENTER**

0000 ALESSANDRO BLVD.  
 MARCH JPA, CALIFORNIA

PROFICIENCY 215 LLC  
 11777 SAN VICENTE BLVD.,  
 SUITE 700  
 LOS ANGELES, CA 90049  
 P 310-979-8000  
 F 310-979-1772




NO.	DATE	DESCRIPTION
01		CONSTRUCTION ISSUE
02		NO SET
03		PLAN CHECK SUBMITTAL
04		DESIGN DEVELOPMENT
05		SCHEMATIC DESIGN
06		NO
07		NO

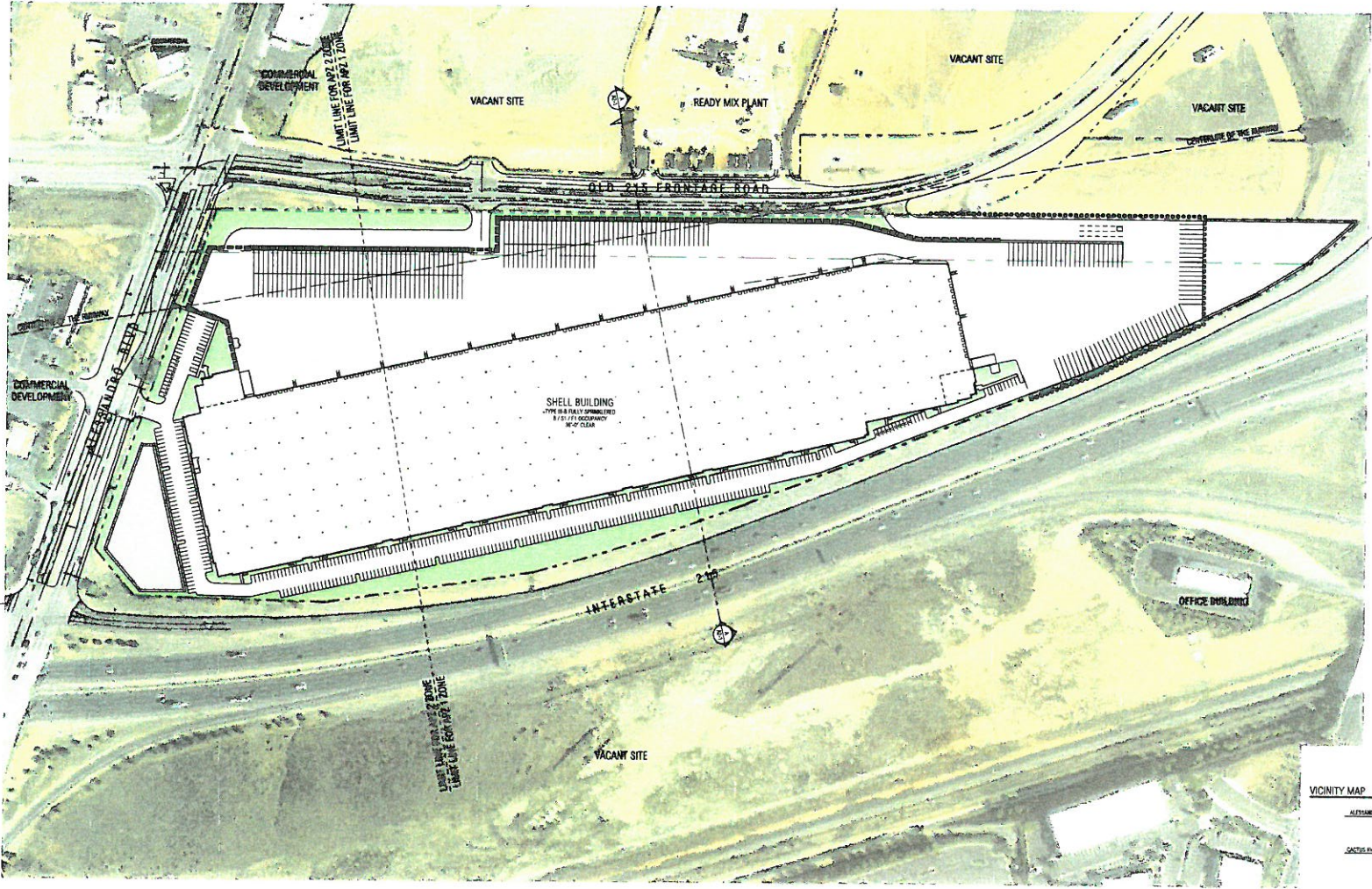
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 OWNER PROJECT NO: 0300-01  
 CAD FILE NAME: 1400-01-A-1  
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 CHECKED BY: ME  
 COPYRIGHT: REGA OFFICE OF ARCHITECTURAL DESIGN  
 SHEET TITLE: SITE PLAN

NO.	DATE	DESCRIPTION
CO		CONSTRUCTION ISSUE
RD		R/D SET
PC		PLAN CHECK SUBMITTAL
DO		DESIGN DEVELOPMENT
SD		SCHEMATIC DESIGN
MA		MARK

RG PROJECT NO.	14208-00
OWNER PROJECT NO.	0000-00
CAD FILE NAME	14208-00-01-00
DRAWN BY	MG
CHECK BY	IG
DATE	02/13/20
COMPILED BY	
PROJECT	RG, OFFICE OF ARCHITECTURAL DESIGN
SHEET TITLE	SITE SCREENING PLAN

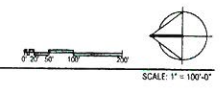
- SCREENING LEGEND**
-  4" BLACK PAINTED TUBULAR STEEL POSTS WITH PERFORATED STEEL END CAPS. PERFORATED STEEL END CAPS WITH STONE CLAD ON THE OUTSIDE. SAND & GRAIN COLORED CAP PLATES AT 8" O.C. MAX.
  -  4" WATER QUALITY BARS PROVIDE 4" BLACK PAINTED TUBULAR STEEL POSTS WITH STONE CLAD ON THE OUTSIDE. PERFORATED STEEL END CAPS WITH SAND & GRAIN COLORED CAP PLATES AT 8" O.C. MAX.
  -  12" PAINTED METALON TUBULAR SCREEN WALL WITH STONE CLAD ON THE OUTSIDE. PERFORATED STEEL END CAPS WITH SAND & GRAIN COLORED CAP PLATES AT 8" O.C. MAX.



VICINITY MAP



SITE SCREENING PLAN



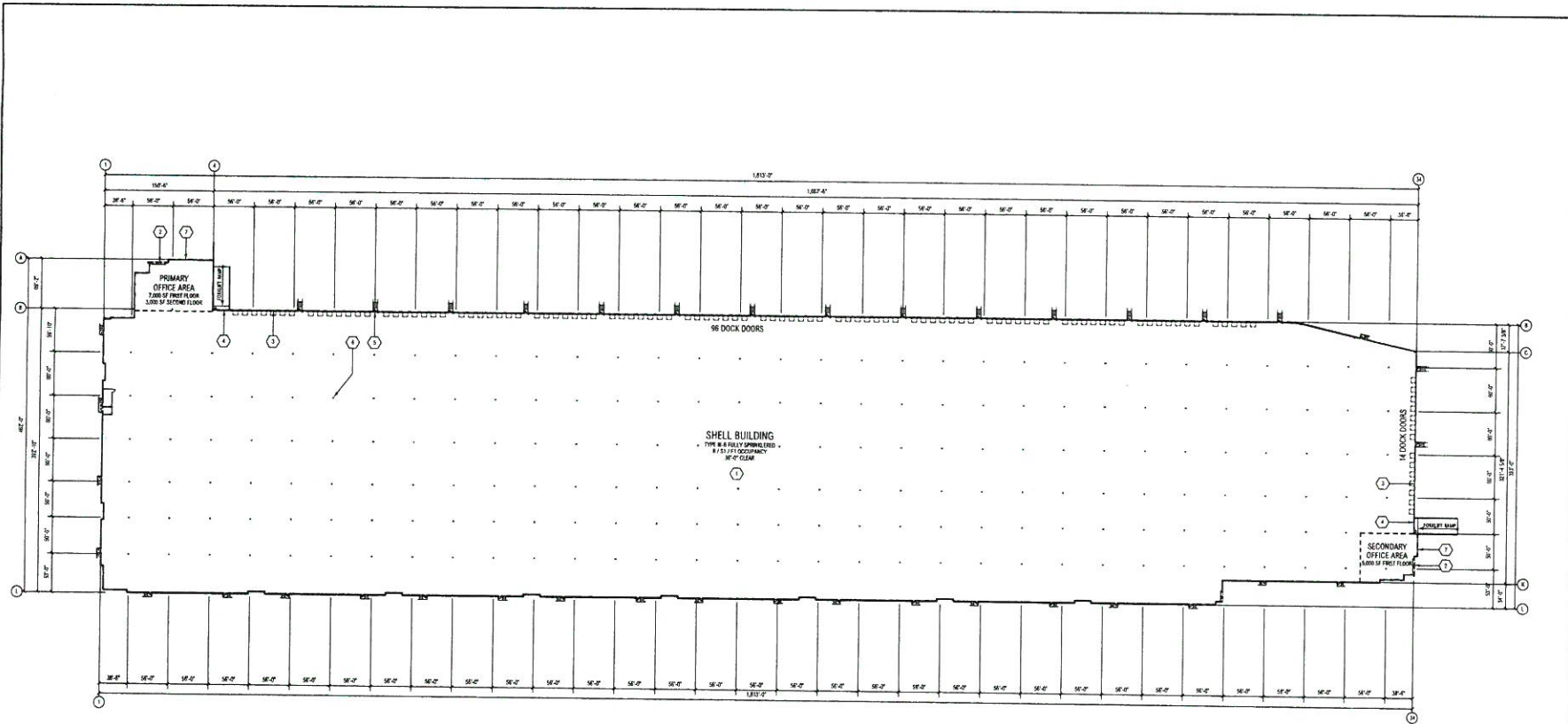
CONTRACT

PROFESSIONAL SEAL

**FREEWAY BUSINESS CENTER**

0000 ALESSANDRO BLVD.  
MARCH JPA, CALIFORNIA

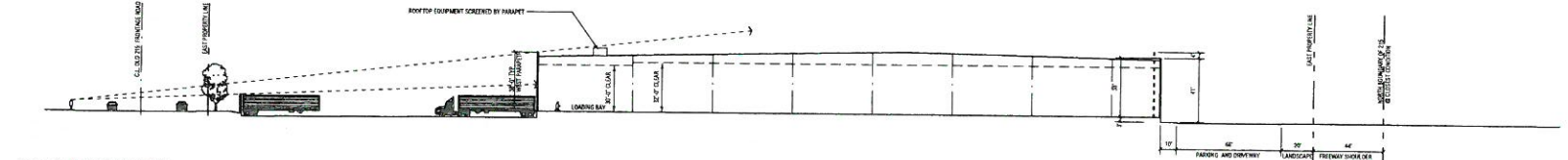
PROFICIENCY 215 LLC  
11777 SAN VICENTE BLVD.,  
SUITE 700,  
LOS ANGELES, CA 90049  
P 310-879-8000  
FX 310-879-7772



FLOOR PLAN

**KEYNOTES**

- 1. PAINTED CONCRETE FRT. OF WAREHOUSE / OFFICE / MANUFACTURING FACILITY
- 2. PAINTED FINISHES
- 3. PAINTED 8' X 10' DOOR WITH METAL TRUCK DOORS.
- 4. PAINTED 12' X 14' GRADE LEVEL METAL TRUCK DOORS.
- 5. PAINTED 5' X 7' METAL ACCESS BAY DOORS.
- 6. STRUCTURAL BUILDING COLUMN.
- 7. STRUCTURE GLAZING SET IN CLEAR ANODIZED ALUMINUM 2" X 4 1/4" MAX. OFF-SET GLAZING SYSTEM.



SITE / BUILDING SECTION

NO.	DATE	DESCRIPTION

NO.	DATE	DESCRIPTION
CD		CONSTRUCTION ISSUE
BD		BD SET
PC		PLAN CHECK SUBMITTAL
SD		SCHEMATIC DEVELOPMENT
SD	11/16/15	SCHEMATIC DESIGN
MARK		DESCRIPTION

RGD PROJECT NO.	14220-00
OWNER PROJECT NO.	0000-00
CAD FILE NAME	14220-00-A2-1
DRAWN BY	ME
CHECK BY	ME
COMPANIT	RGD, OFFICE OF ARCHITECTURAL DESIGN



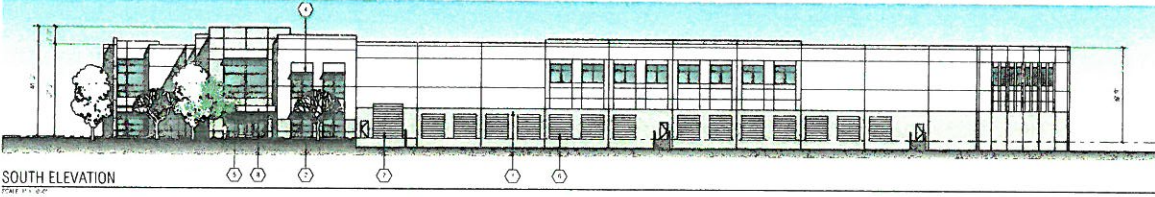
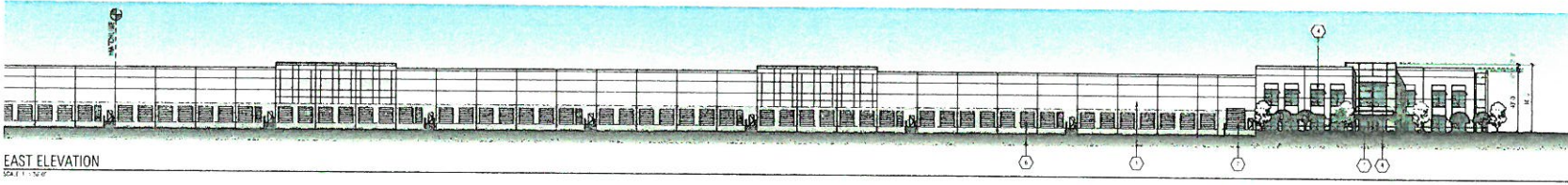
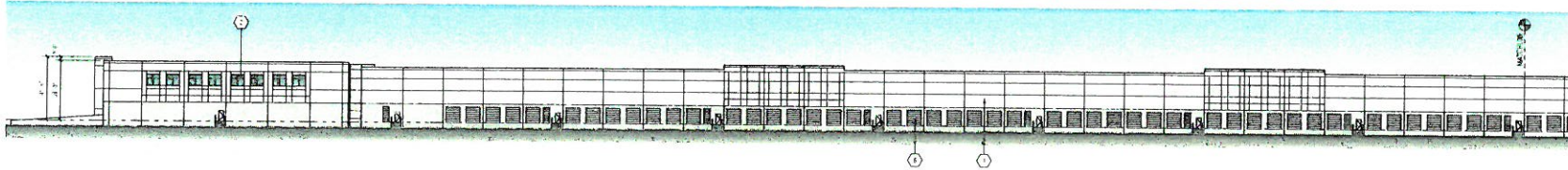
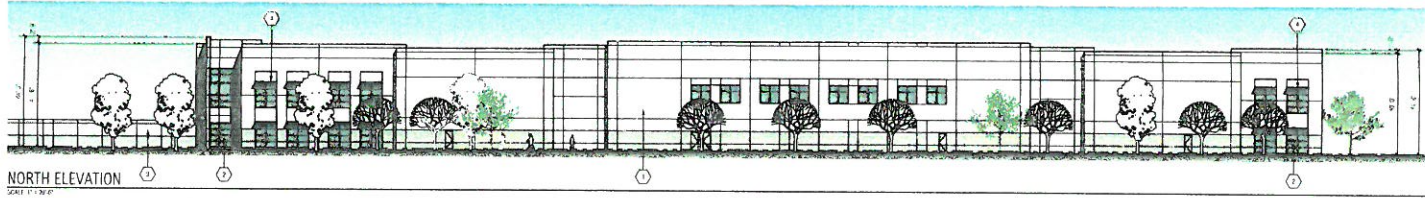
CONSULTANT

PROFESSIONAL SEAL

## FREEWAY BUSINESS CENTER

0000 ALESSANDRO BLVD.  
MARCH JPA, CALIFORNIA

PROFICIENCY 215 LLC  
11777 SAN VICENTE BLVD.,  
SUITE 780  
LOS ANGELES, CA 90049  
P 310-979-0000  
F 310-979-7772



- #### FINISH SCHEDULE
- 1. FLOOR COVER - GLOSSON PANELS - ALL OVER WHITEBOARD
  - 2. 1/4" x 1/4" METAL FLOOR - GLOSSON PANELS - MATCH LEFT OVER
  - 3. DRINK FOUNTAIN ON 3R - GLOSSON PANELS - MATCH HANDICAP SYMBOL
  - 4. 1/4" x 1/4" GLASSING - SEE ALIGNED - 1/4" x 1/4" BRUSH GLAZING - SEE FINISHES FOR LOCATION OF INSTALLATION
- #### KEYNOTES
- 1. FINISHES CONSISTENT WITH PANELS TO ACCESS PANELS AS SHOWN
  - 2. REFLECTIVE BLUE GLASS PANELS CONTROLLED ALUMINUM MESH ON SYSTEM
  - 3. ALUMINUM FINISHED CHANNELS OVER ENTRY ELEMENTS
  - 4. METAL SUBSTRATE OVER EXPOSED ELEMENTS AND JOINTS
  - 5. RECEIVED ENTRY FROM FINISHES PLUS A DRAINAGE GROUND
  - 6. PARTITION 5/8" x 1/4" THICK HIGH-MODULUS LIFT METAL TRUCK DOOR ASSEMBLY WITH 1000 X BARRIERS - SEE DOOR SCHEDULE
  - 7. PARTITION 1/4" x 1/4" GROUND LEVEL VERTICAL OPEN METAL BRIDGE DOOR ASSEMBLY - SEE DOOR SCHEDULE
  - 8. 1/4" x 1/4" GLASSING MATERIAL AT OFFICE ENTRY ELEMENTS
  - 9. FORMER TRUCK DOOR SYSTEM - PARTIAL AND RE-USE GLASSING TO MATCH BLUE TINT

NO.	DATE	DESCRIPTION

PROJECT NO.	1448010
OWNER PROJECT NO.	1448010
CAD FILE NAME	1448010_1.rvt
DRAWN BY	RG
CHECKED BY	DE
COMPILED BY	RG&A OFFICE OF ARCHITECTURE DESIGN
SHEET TITLE	EXTERIOR ELEVATIONS

**GENERAL NOTES:**

- ALL WORK DONE FOR THESE DRAINAGES SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, LATEST EDITION, INCLUDING SUPPLEMENTS.
- ALL CONTRACTORS PERFORMING WORK ON THIS PROJECT SHALL FAMILIARIZE THEMSELVES WITH THE SITE AND SHALL BE SOLELY RESPONSIBLE FOR ALL DAMAGE TO EXISTING FACILITIES RESULTING DIRECTLY OR INDIRECTLY FROM THEIR OPERATIONS, WHETHER OR NOT SHOWN ON THESE DRAWINGS.
- IN CASE OF ANY ACCIDENTS INVOLVING SAFETY MATTERS COVERED BY SECTION 6504 OF THE CALIFORNIA LABOR CODE, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE STATE DIVISION OF INDUSTRIAL SAFETY.
- FOR DEVELOPMENT SITE, THE OWNER SHALL FILE A NOTICE OF BECKET ALLOW WITH THE APPROPRIATE ANNUAL FEE WITH THE DIVISION OF WATER QUALITY OF THE STATE WATER RESOURCES CONTROL BOARD PRIOR TO COMMENCEMENT OF CONSTRUCTION IN ACCORDANCE WITH FEDERAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (P.D.E.S.) PERMIT REQUIREMENTS.
- FOR SITE OF 1 ACRES OR MORE, A SOIL SHELVING AND REUSE STUDY AND A GSP SHALL BE ASSIGNED RESPONSIBILITY FOR NON-STORM WATER VISUAL OBSERVATIONS, SAMPLING AND ANALYSIS OF ALL ELEMENTS OF THE STUDY, INCLUDING THE PREPARATION OF THE ANNUAL COMPLIANCE EVALUATION AND THE ELABORATION OF ALL UNAUTHORIZED DISCHARGES, UNLESS A REVIEW IS APPROVED.
- NO ROCK OR OTHER UNDESIRABLE MATERIAL WITH A MAXIMUM DIMENSION GREATER THAN 8 INCHES IN DIAMETER SHALL BE PLACED IN THE FILL UNLESS RECOMMENDATIONS FOR SUCH PLACEMENT HAVE BEEN SUBMITTED BY THE SOIL ENGINEER AND APPROVED IN ADVANCE BY THE BUILDING OFFICIAL.
- THE FIELD ENGINEER MUST SET GRADE STAKES FOR ALL DRAINAGE DEVICES.
- PROVISIONS SHALL BE MADE FOR CONTRIBUTORY DRAINAGE AT ALL TIMES.
- THE CONTRACTOR SHALL CONTACT ALL UTILITY COMPANIES AS REQUIRED, 48 HOURS PRIOR TO EXCAVATION.
- AGGREGATE BASE SHALL BE CRUSHED AGGREGATE BASE AS DEFINED IN SECTION 200-2.2 OF THE "STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION," UNLESS INDICATED OTHERWISE.
- CONCRETE CURBS, GUTTERS, WALKS, APPROXS AND PAVEMENT SHALL BE CLASS 500-C-2500 MIX PORTLAND CEMENT CONCRETE AS DEFINED IN SECTION 201-1 OF THE "STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION," UNLESS INDICATED OTHERWISE.
- THE TOP 8 INCHES OF SURGRADE MATERIAL SHALL BE COMPACTED TO A RELATIVE DENSITY OF 95 PERCENT WHEN PAVEMENT IS TO BE PLACED DIRECTLY ON SURGRADE MATERIAL.
- THE TOP 6 INCHES OF SURGRADE MATERIAL SHALL BE COMPACTED TO A RELATIVE DENSITY OF 90 PERCENT WHEN BASE OR SURGRADE MATERIAL, CURB, GUTTER OR SIDEWALK ARE PLACED ON THE SURGRADE MATERIAL.
- ALL AGGREGATE BASE SHALL BE COMPACTED TO A MINIMUM RELATIVE DENSITY OF 95 PERCENT.
- ALL PAVEMENT AND CONCRETE REMOVALS SHALL BE SAWCUT.
- PROPOSED CONCRETE WALKWAYS AND LANDSCAPING HARDSCAPE SHALL BE CONSTRUCTED AS SPECIFIED PER ARCHITECTURAL DRAWINGS, UNLESS SPECIFICALLY INDICATED OTHERWISE PER THESE DRAWINGS.
- ALL PCC CONCRETE SURFACES SHALL BE PER ARCHITECTURAL DRAWINGS.
- THE CONTRACTOR SHALL VERIFY ALL JOB ELEVATIONS AND LOCATIONS AT THE START OF THE CONSTRUCTION. IF DRAINING RECORDS ARE REQUIRED, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER, THE LOCATIONS AND ELEVATION OF THE JOB, FOR THE ENGINEER TO REVISION THE DRAWINGS.

# GRADING AND DRAINAGE PLAN

FOR

## FREEWAY BUSINESS CENTER

MARCH JPA, CALIFORNIA

**PRIVATE ENGINEER'S NOTICE TO CONTRACTORS:**

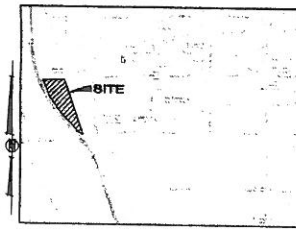
THE EXISTENCE AND APPROXIMATE LOCATION OF UNDEVELOPED UTILITIES IS INDICATED BY SHOWN ON THESE DRAWINGS AND CONSIDERED AS A BASIS OF AVAILABLE RECORDS TO THE BEST OF OUR KNOWLEDGE AND BELIEF. THE CONTRACTOR SHALL BE RESPONSIBLE TO VERIFY THE LOCATION AND DEPTH OF ALL UTILITIES SHOWN AND ALL OTHER UTILITIES LINES OF STRUCTURES NOT SHOWN ON THESE DRAWINGS.

THESE DRAWINGS DO NOT RELIEVE THE CONTRACTOR AND SUBCONTRACTORS FROM RESPONSIBILITY FOR THE CORRECTION OF ERRORS AND OMISSIONS INCURRED DURING CONSTRUCTION.

ALL CONTRACTORS AND SUBCONTRACTORS PERFORMING WORK SHOWN OR RELATED TO THESE DRAWINGS SHALL CONSIDER THE OPERATIONS TO BE FULLY RESPONSIBLE AND SHALL BE PLACED TO WORK AND THE PUBLIC IS PROTECTED. ALL CONTRACTORS AND SUB- CONTRACTORS SHALL COMPLY WITH THE OCCUPATIONAL SAFETY AND HEALTH REGULATIONS OF THE U.S. DEPARTMENT OF LABOR AND OSHA.

THE FIELD ENGINEER SHALL NOT BE RESPONSIBLE IN ANY WAY FOR THE CONTRACTOR AND SUBCONTRACTOR COMPLIANCE WITH THE OCCUPATIONAL SAFETY AND HEALTH REGULATIONS OF THE U.S. DEPARTMENT OF LABOR OR WITH THE STATE OF CALIFORNIA DEPARTMENT OF INDUSTRIAL RELATIONS "CONSTRUCTION SAFETY ORDERS."

CONTRACTOR AGREES THAT HE WILL BE RESPONSIBLE TO ASSUME FULL AND COMPLETE RESPONSIBILITY FOR THE CORRECTION OF ERRORS AND OMISSIONS INCURRED DURING CONSTRUCTION. CONTRACTOR GENERAL POWER AND THE SAFETY OF ALL PERSONS AND PROPERTY THAT THE CONTRACTOR SHALL ASSUME CONTROL AND BE HELD LIABLE TO NORMAL PERSONS UNDER AND THAT THE CONTRACTOR SHALL OBTAIN, MAINTAIN AND HOLD THE INSURANCE AND THE CITY OF PERRIS WAIVERED FROM ANY AND ALL LIABILITY, BEING RELEASED IN CONNECTION WITH THE PERFORMANCE OF WORK ON THE PROJECT, EXCEPT LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE ENGINEER.



**VICINITY MAP**

NO SCALE  
 THE THOMAS QUARTER, INVEREDGE COUNTY (2008 EDITION) PAGE 714 DWD 3  
 SECTION: 15 TOWNSHIP: 35 RANGE: 4W

**ASSESSOR'S PARCEL NO.**  
 297-100-043-6  
 297-100-013

**BASIS OF BEARING:**  
 THE BEARINGS SHOWN HEREON ARE BASED ON THE BEARING OF NORTH OR S47°00' WEST FOR THE CENTERLINE OF CACTUS AVENUE AS SHOWN ON RECORD OF SURVEY 00-133, ON FILE IN BOOK 110, PAGES 39 AND 40, INCLUSIVE OF RECORDS OF SURVEY, RECORDS OF INVEREDGE COUNTY, CALIFORNIA.

**OWNER/DEVELOPER:**  
 PROFICIENCY 215 LLC  
 11777 SAN VICENTE BLVD., STE. 700  
 L.A., CA 90049  
 ATTN: MR. JEFFREY M. WHEATON  
 PH: (310) 978-8000 / FAX (310) 978-7772

**ARCHITECT:**  
 R.C.A. OFFICE OF ARCHITECTURAL DESIGN  
 15321 ALTON PARK WAY STE 100  
 POME, CA 92516  
 ATTN: MR. MICHAEL DELL  
 PH: (949) 341-0920 / FAX (949) 341-0922

**SOIL ENGINEER:**  
 MORCAL ENGINEERING  
 10641 HAMBOLDT STREET  
 LOS ANGELES, CA 90020  
 ATTN: MR. MARCO BERENSON  
 PH: (323) 799-8400 / FAX (323) 799-8450

**CIVIL ENGINEER:**  
 THOMSEN ENGINEERING, INC.  
 10811 E. GALE AVE.  
 INVEREDGE, CA 91748  
 ATTN: MR. ROBERT SULLIVAN  
 PH: (925) 945-8350 x 104

**INDEX TO SHEETS**

SHEET NO.	DESCRIPTION
1	Title Sheet
2	DETAILS
3	KEY MAP
4	PLAN A
5	PLAN B
6	PLAN C
7	PLAN D
8	PLAN E
9	PLAN F
10	PLAN G
11	UTILITIES - NORTH
12	UTILITIES - SOUTH

- LEGEND:**
- ⊙ CONSTRUCTION NOTE REFERENCE
  - EXISTING ELEVATION
  - (E) EXISTING
  - CENTERLINE
  - PROPERTY LINE
  - PLAN DIRECTION AND SLOPE ANGLE
  - ⊠ HORIZONTAL PARKING STALL DESIGNATION
  - DRIVE LINE FENCE (ONLY)
  - CONCRETE SURFACE (PROPOSED)
  - RETAINING WALL (PROPOSED)
  - TOP OF GRADED SURFACE (PROPOSED)
  - ⊠ FIRE HYDRANT (PROPOSED)
  - ⊠ FIRE HYDRANT (EXISTING)
  - ⊠ GROUND POST (PROPOSED)
  - ⊠ GROUND POST (EXISTING)
  - ⊠ LIGHT OR SIGN (EXISTING)
  - ⊠ TRAFFIC SIGN
  - ⊠ ROOF DRAIN OUTLET

**LIST OF ABBREVIATION:**

- AC ASPHALT CONCRETE
- BB BACK OF BULK
- BS BULK HEAD
- CF CURB FACE
- CONC CONCRETE
- ELEC ELECTRICAL
- FL FLOOR FINISH
- FL FLOOR FINISH
- FRM FORM CONCRETE
- GV GAS VALVE
- HW HORIZONTAL
- GP GATE POST / GROUND POST
- MC PORTLAND CEMENT CONCRETE
- PL PLASTER
- PI POST INDICATOR MARK
- RY ROOF RISE
- SD STAIR STRING
- SPR STAIR STRING CLEAN OUT
- STMP STAIR STRING MARKER
- S SOLAR PANEL
- SWITCH SWITCH
- TRC TRUNK MANHOLE
- UT UTILITIES LIGHT
- VP TOP OF CURB
- WEL WELDER
- WEL WELDER
- WEL TOP OF DRIVE
- WEL WATER MAIN

**NOTIFICATION**

CONTRACTOR SHALL NOTIFY THE FOLLOWING UTILITIES COMPANIES AND AGENCIES AT LEAST 48 HOURS PRIOR TO COMMENCING CONSTRUCTION.

EASTERN MUNICIPAL WATER DISTRICT	(951) 928-3777 EXT. 4030
UNDERGROUND SERVICE ALERT	(800) 227-2600
SOUTHERN CALIFORNIA EDISON CO.	(800) 227-2600
SOUTHERN CALIFORNIA GAS CO.	(800) 227-2600
TELEPHONE COMPANY	(800) 227-2600
INVEREDGE COUNTY FLOOD CONTROL DIST.	(951) 955-1288

PLUS ALL PERMIT AGENCIES

**APPROXIMATE EARTHWORK QUANTITIES:**

THE EARTHWORK QUANTITIES SHOWN ON THIS PLAN DO NOT REFLECT ANY SHORTRAGE, SHELTING, SUBSIDENCE OR ANY SPECIAL CONDITIONS THAT MAY BE SPECIFIED IN THE PRELIMINARY SOILS REPORT AND ARE FOR INFORMATION AND FEE PURPOSES ONLY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING HIS OWN EARTHWORK QUANTITIES FOR BIDDING, CONTRACT AND CONSTRUCTION PURPOSES.

DIG: \_\_\_\_\_ CU YD, FILL: \_\_\_\_\_ CU YD.  
 OVEREXCAVATION: \_\_\_\_\_ CU YD, IMPORT: \_\_\_\_\_ CU YD.

**DIGALERT**

DIAL BEFORE YOU DIG

TWO WORKING DAYS BEFORE YOU DIG

TOLL FREE 1-800-227-2600

A PUBLIC SERVICE BY UNDERGROUND SERVICE ALERT

**NOTE:**  
 WORK CONTAINED WITHIN THESE PLANS SHALL NOT COMMENCE UNTIL AN ENFORCEMENT PERMIT AND/OR A GRADING PERMIT HAS BEEN ISSUED.

The private engineer accepts these plans as responsible for accuracy and completeness of the data shown. In the event of discrepancies within the data supplied or during construction, the private engineer shall be responsible for obtaining or completing sufficient and reliable data prior to approval by the city.

MARK	BY	DATE	REVISIONS	APPR.	DATE
	ENGINEER				CITY

**MARCH JOINT POWERS AUTHORITY**

APPROVED BY: \_\_\_\_\_

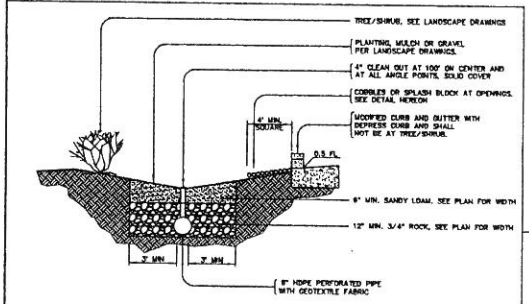
DIRECTOR OF PUBLIC WORKS \_\_\_\_\_ DATE \_\_\_\_\_



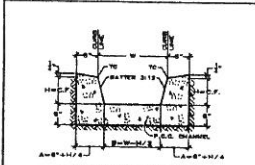
**Thomsen Engineering, Inc.**  
 Civil Engineering, Land Planning  
 8 Land Resources  
 18811 E. Gale Ave.  
 Inveredge, CA 91748  
 Tel: (925) 945-8350  
 Fax: (925) 945-8350

**BENCHMARK:**  
 INVEREDGE COUNTY BENCH MARK 30-30, 1951  
 ELEVATION = 544.00 FEET ± 0.00 FT  
 AT THE INTERSECTION OF PERRIS AVENUE AND GARDNER STREET (ELEVATION IN FEET) WEST OF PERRIS AVENUE. THE POST SHOWS 0.00 FEET OF PERRIS AVENUE. A BENCH MARK IS 48 FEET HIGHER. A BENCH MARK IS SET ON THE TOP OF CURB AT RECORDING NUMBER 1043.

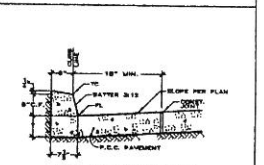
SHEET NO. 1		OF 12 SHEETS	
PREPARED BY: HANS C. THOMSEN R.C.E. NO. 8244		DATE: 02/08/2014	
SCALE: H: N/A V: N/A		FOR: PROFICIENCY 215 LLC	
CITY FILE NO. _____		TOWNSHIP: 35 RANGE: 4W	



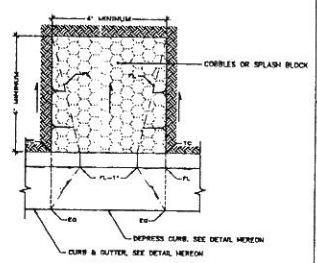
FILTRATION / INFILTRATION TRENCH DETAIL



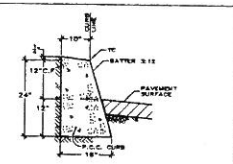
CHANNEL DETAIL



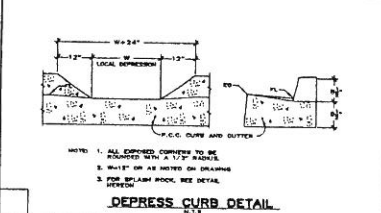
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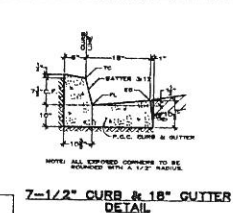
SPLASH ROCK DETAIL



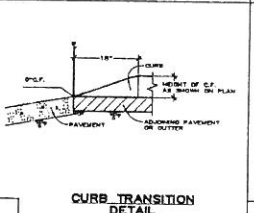
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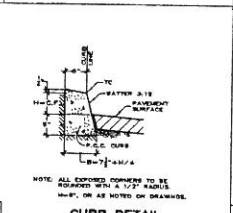
DEPRESS CURB DETAIL



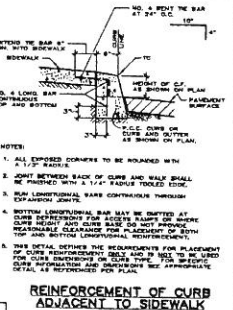
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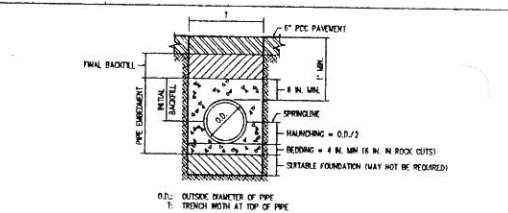
CURB TRANSITION DETAIL



CURB DETAIL

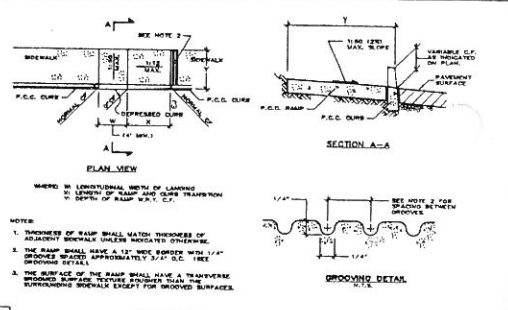


REINFORCEMENT OF CURB ADJACENT TO SIDEWALKS DETAIL



**HOPE PIPE BEDDING DETAIL**

NOTES:  
 EMBEDEDMENT AND FINAL BACKFILL MATERIALS SHALL BE SUITABLE GRANULAR MATERIAL CONFORMING TO ASTM D 2321 CLASS 1 OR 2.  
 INSTALLATION OF BEDDING SHALL BE PER THE MANUFACTURER'S RECOMMENDATIONS AND COMPLY WITH SECTION 306-1.2 OF THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION.  
 EMBEDEDMENT MATERIAL INCLUDING BEDDING, HANDSOMING AND FINAL BACKFILL SHALL BE PLACED IN 8-IN. MAX. LIFT AND COMPACTED TO 90 PERCENT STANDARD PROCTOR DENSITY.  
 FINAL BACKFILL SHALL BE PLACED IN 8-IN. MAX. LIFT AND COMPACTED TO 95 PERCENT STANDARD PROCTOR DENSITY.  
 SPECIAL FOUNDATION MATERIAL SHALL BE INSTALLED IN THE CASE OF OVER-ELEVATION AND/OR AN UNSTABLE TRENCH BOTTOM.  
 EXCAVATION FROM 0 TO 12 INCHES BELOW PIPE SHALL BE FILLED WITH SUITABLE WOODING MATERIAL PLACED IN 8-IN. MAX. LIFT AND COMPACTED TO A MINIMUM 90 PERCENT PROCTOR DENSITY. AREAS OF OVER-EXCAVATION BEYOND 12 INCHES BELOW PIPE SHALL BE FILLED WITH PROCESSED STONE OR GRAVEL.  
 BEDDING MATERIAL SHALL BE LEVELLED TO FINAL GRADE BY HAND.  
 EMBEDEDMENT MATERIAL SHALL BE WORKED IN AND COMPACTED UNDER THE HANDSOMING IN A MANNER THAT WILL ASSURE COMPLETE CONTACT AND UNIFORM SUPPORT WITH THE PIPE BOTTOM AND FILL Voids BELOW THE PIPE.  
 UNDERLAY TRENCH WIDTH (T) AT TOP OF PIPE: 18" x 24", 110" x 20", 118" x 20", 120" x 20", 126" x 20" OR PER MANUFACTURER REQUIREMENTS.  
 REFERENCE: ASTM STANDARD D2321



HANDICAP ACCESS RAMP DETAIL

NOTE:  
 WORK CONTAINED WITHIN THESE PLANS SHALL NOT COMMENCE UNTIL AN ENCROACHMENT PERMIT AND/OR A GRADING PERMIT HAS BEEN ISSUED.

The public engineer certifies that plans are prepared in conformity with the provisions of the Public Works Act and the regulations thereunder and that the plans are correct and complete as shown and that the work to be done thereunder will conform to the provisions of the Public Works Act and the regulations thereunder.

MARK BY DATE	REVISIONS	APPR. DATE
ENGINEER		CITY

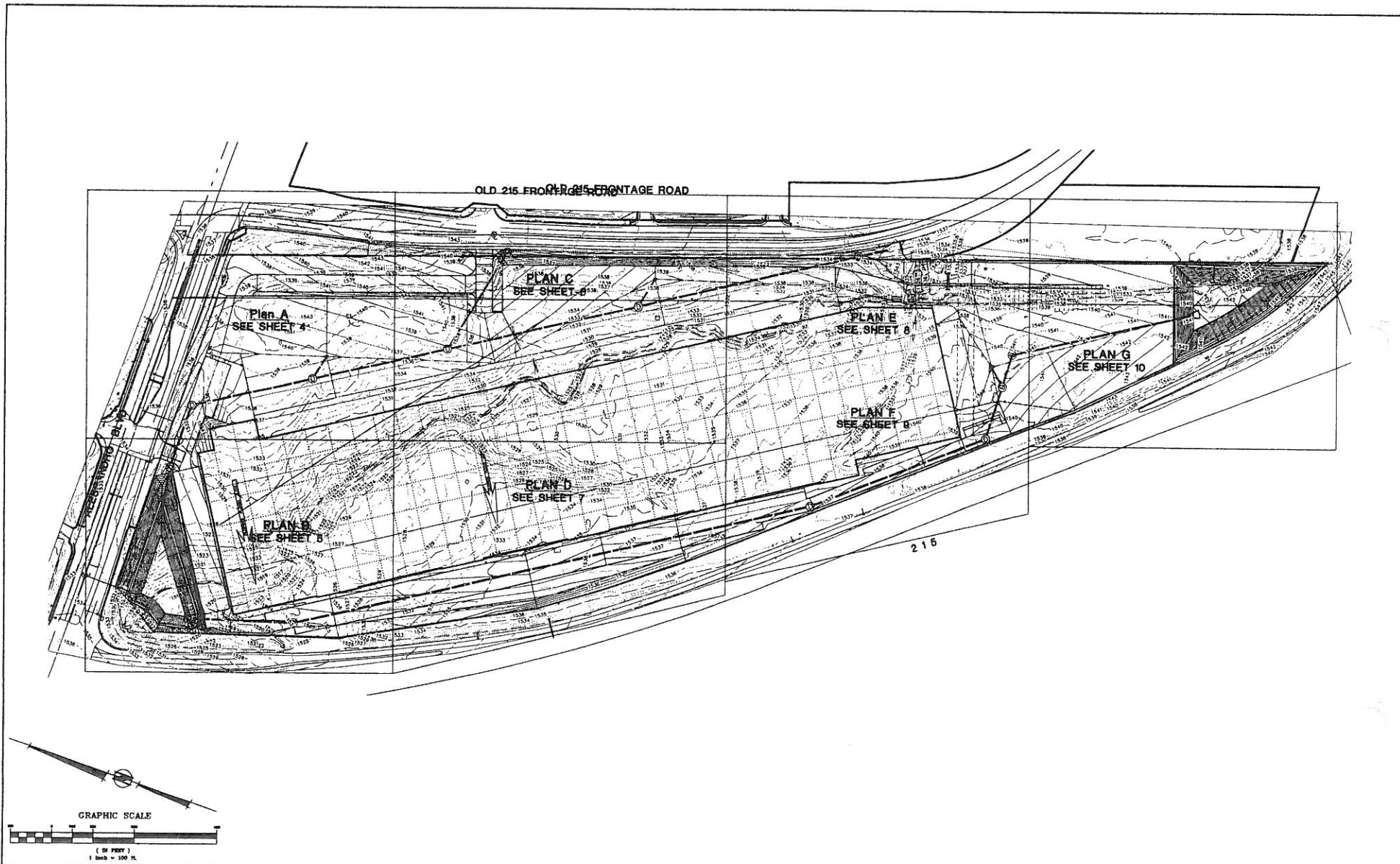
MARCH JOINT POWERS AUTHORITY  
 APPROVED BY: \_\_\_\_\_  
 DIRECTOR OF PUBLIC WORKS DATE \_\_\_\_\_



Thomsen Engineering, Inc.  
 Civil Engineering, Land Planning  
 & Land Surveying  
 1801 E. Oak Ave. Tel: (503) 966-8900  
 Industry, CA 91748 Fax: (503) 966-8276

PREPARED BY: HANS C. THOMSEN R.C.E. NO. 8244 DATE \_\_\_\_\_  
 82238, 09/30/2014 - For Submittal Review  
 82238

BENCHMARK:	MARCH JPA	SHEET NO.
	PRECISE GRADING PLAN	2
	FREWAY BUSINESS CENTER	OF 12 SHEETS
SCALE: HAS SHOWN, N/A	FOR: Proficiency 215 LLC	CITY FILE NO. _____



**NOTE:**  
 WORK CONTAINED WITHIN THESE PLANS SHALL NOT COMMENCE UNTIL AN ENCROACHMENT PERMIT AND/OR A GRADING PERMIT HAS BEEN ISSUED.

The planee engineer displaying these plans is responsible for verifying the accuracy and availability of the data herein. In the event of discrepancies arising after city approval or during construction, the planee engineer shall be responsible for determining an acceptable solution and notifying the planee by approval by the city.

MARK BY	DATE	REVISIONS	APPR.	DATE	QTY

**MARCH JOINT POWERS AUTHORITY**

APPROVED BY: \_\_\_\_\_  
 DIRECTOR OF PUBLIC WORKS



**Thomsen Engineering, Inc.**  
 Civil Engineering, Land Planning  
 & Land Surveying  
 16642 S. Oak Ave. Tel: (909) 966-8992  
 Industry, CA 91748 Fax: (909) 966-8379

PREPARED BY: HANS C. THOMSEN R.C.E. NO. 8244  
 8/23/08, 09/30/2014 - FOR SUBMITTAL REVIEW

**BENCHMARK:**

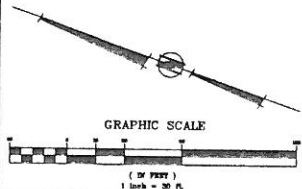
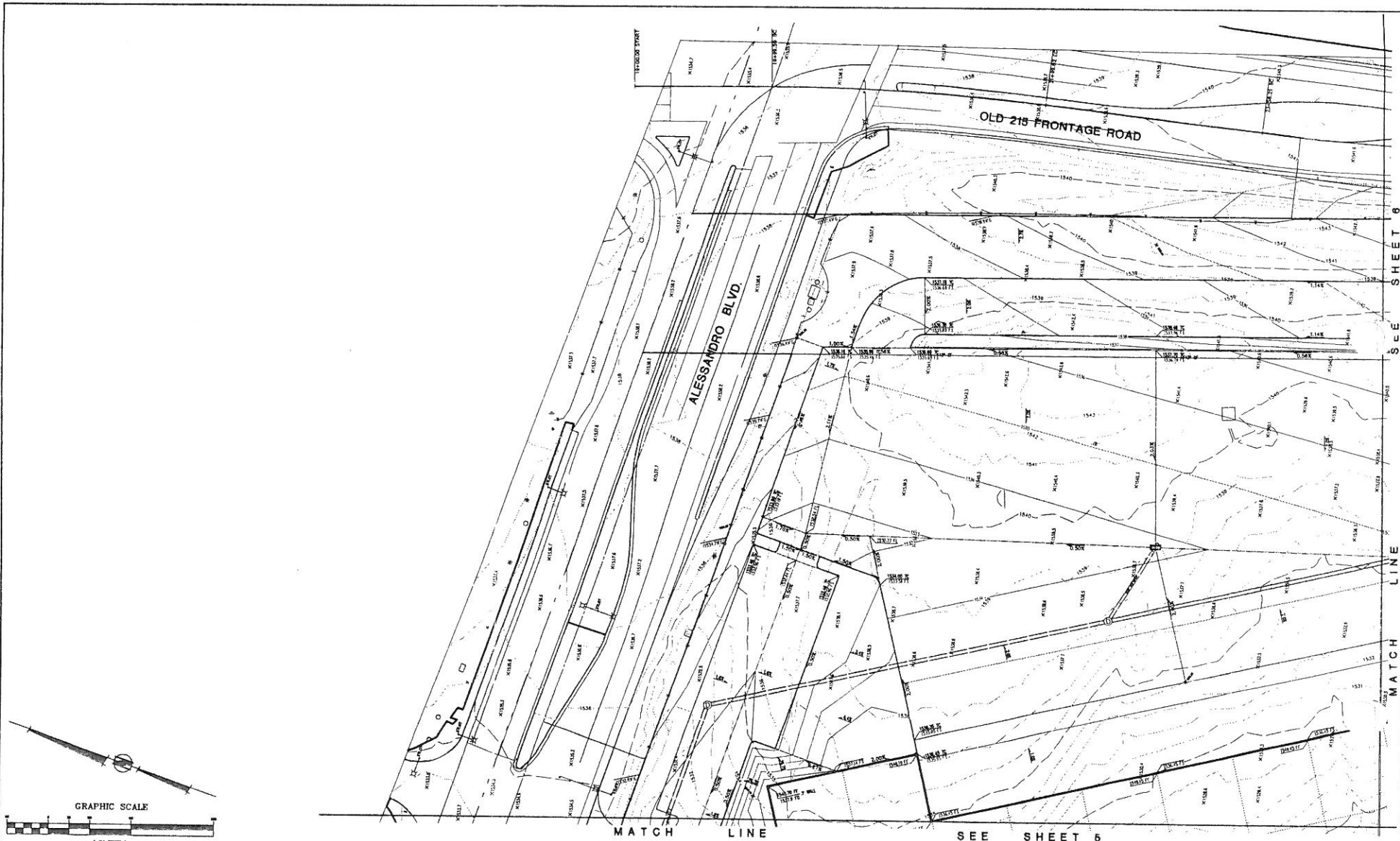
INDUSTRY COUNTY GRASS DOME, N-20, 1983  
 ELEVATION = 154.791, 167.00 - 169.00  
 AT THE UNIVERSITY OF PUEBLO BULLHEAD  
 AND GRASS DOME BEARS 24 FEET WEST  
 OF PUEBLO BULLHEAD. ELEVATION OF  
 THE SURFACE OF ONE OF 14 (1) FEET OF  
 REBAR SHALL BE GRASS DOME SET IN THE TOP OF  
 CONCRETE REBAR AND MARKED N-20 1983

SCALE: H:V = 100' V. N/A

**MARCH JPA**  
**PRECISE GRADING PLAN**  
**FREEWAY BUSINESS CENTER**  
**KEY MAP**

FOR: PROPRIETARY 215 LLC W.D. CITY FILE NO. ---

SHEET NO. 3  
 OF 12 SHEETS



NOTE:  
 WORK CONTAINED WITHIN THESE PLANS SHALL NOT COMMENCE UNTIL AN ENCROACHMENT PERMIT AND/OR A GRADING PERMIT HAS BEEN ISSUED.

The project engineer certifies these plans to be prepared by himself or under his direct supervision and that he is a duly licensed professional engineer in the State of California. He certifies that he is responsible for the accuracy of the data and the correctness of the design and that he is not providing engineering services in violation of the laws of the State of California.

MARK	DATE	REVISIONS	APPR.	DATE
ENGINEER			CITY	

**MARCH JOINT POWERS AUTHORITY**  
 APPROVED BY: \_\_\_\_\_  
 DIRECTOR OF PUBLIC WORKS      DATE \_\_\_\_\_



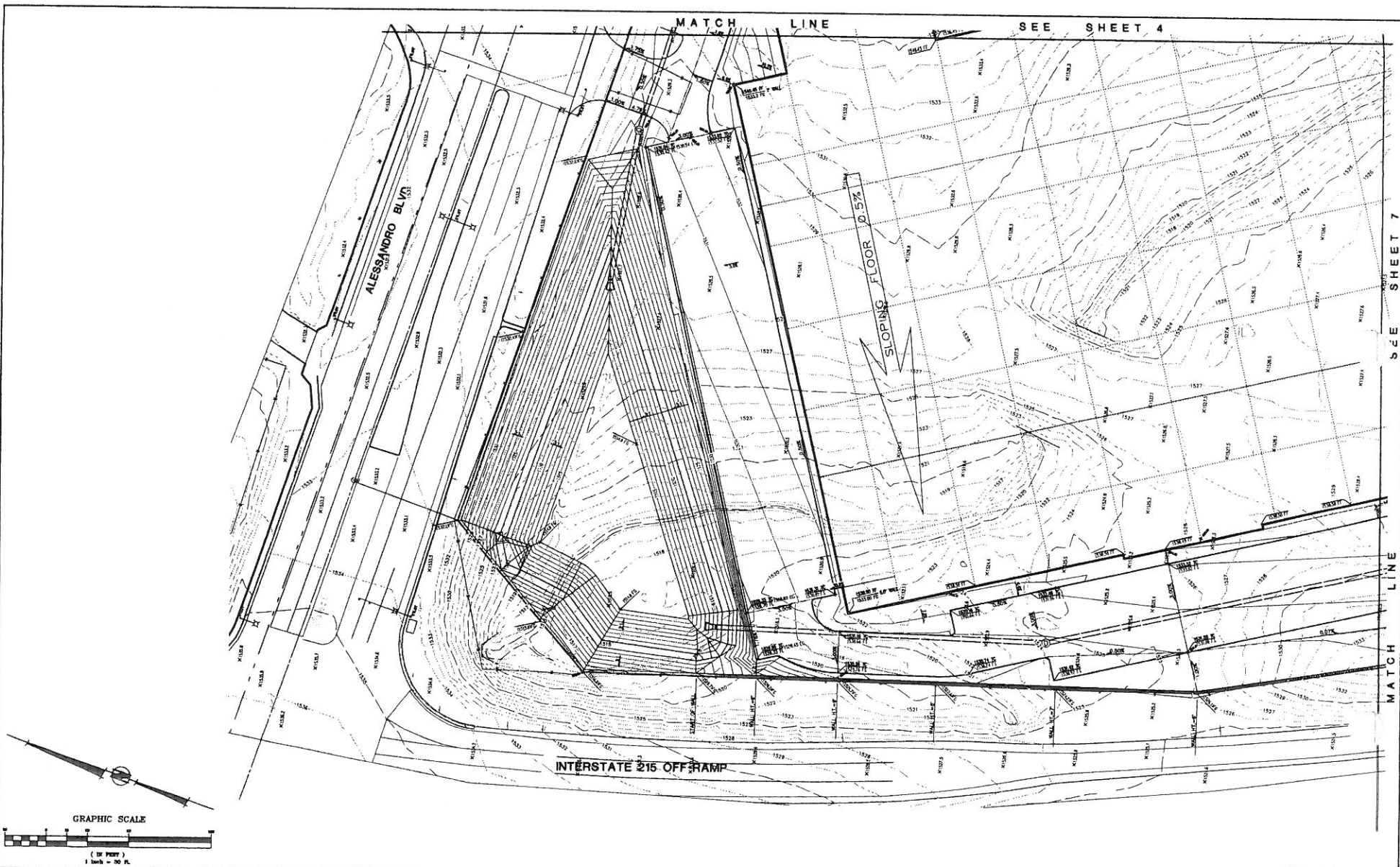
**Thomsen Engineering, Inc.**  
 CIVIL Engineering, Land Planning  
 & Land Surveying  
 18811 E. Gale Ave.      Tel: (909) 886-8900  
 Redlands, CA 91748      Fax: (924) 998-2299

PREPARED BY: HANS C. THOMSEN R.C.E. NO. 8244      DATE \_\_\_\_\_  
 82438-08/30/2014 - FOR SUBMITTAL REVIEW

**BENCHMARK:**  
 BENCHMARK: 215 L.L.C. W.D. 11/11/13  
 ELEVATION = 144.224, DATUM = 1988.28  
 ALL DIMENSIONS SHOWN ARE IN FEET AND INCHES UNLESS OTHERWISE SPECIFIED.  
 THE HORIZONTAL CURVE DATA IS TO BE USED IN CONJUNCTION WITH THE VERTICAL CURVE DATA AND SHOWN IN THE PLAN SHEET.

SCALE: HORIZ. = 1" = 300' V. = 1" = 10'

<b>MARCH JPA</b> <b>PRECISE GRADING PLAN</b> <b>FREEWAY BUSINESS CENTER</b> <b>PLAN A</b>		W.D. NO. _____ SHEET NO. <b>4</b> OF 12 SHEETS
FOR: PROPOGENCY 215 L.L.C.	W.D.	CITY FILE NO. _____



**NOTE:**  
 WORK CONTAINED WITHIN THESE PLANS SHALL NOT COMMENCE UNTIL AN ENCROACHMENT PERMIT AND/OR A GRADING PERMIT HAS BEEN ISSUED.

The private engineer hereby certifies that he is responsible for verifying the accuracy and availability of the design data. In the event of discrepancies arising after the approval or during construction, the private engineer shall be responsible for obtaining or completing additional data and making the plan be corrected by the City.

MARK	BY	DATE	REVISIONS	APPR.	DATE	QTY

**MARCH JOINT POWERS AUTHORITY**  
 APPROVED BY: \_\_\_\_\_  
 DIRECTOR OF PUBLIC WORKS      DATE: \_\_\_\_\_



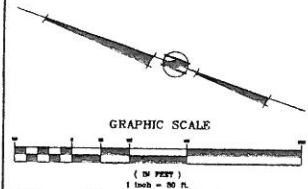
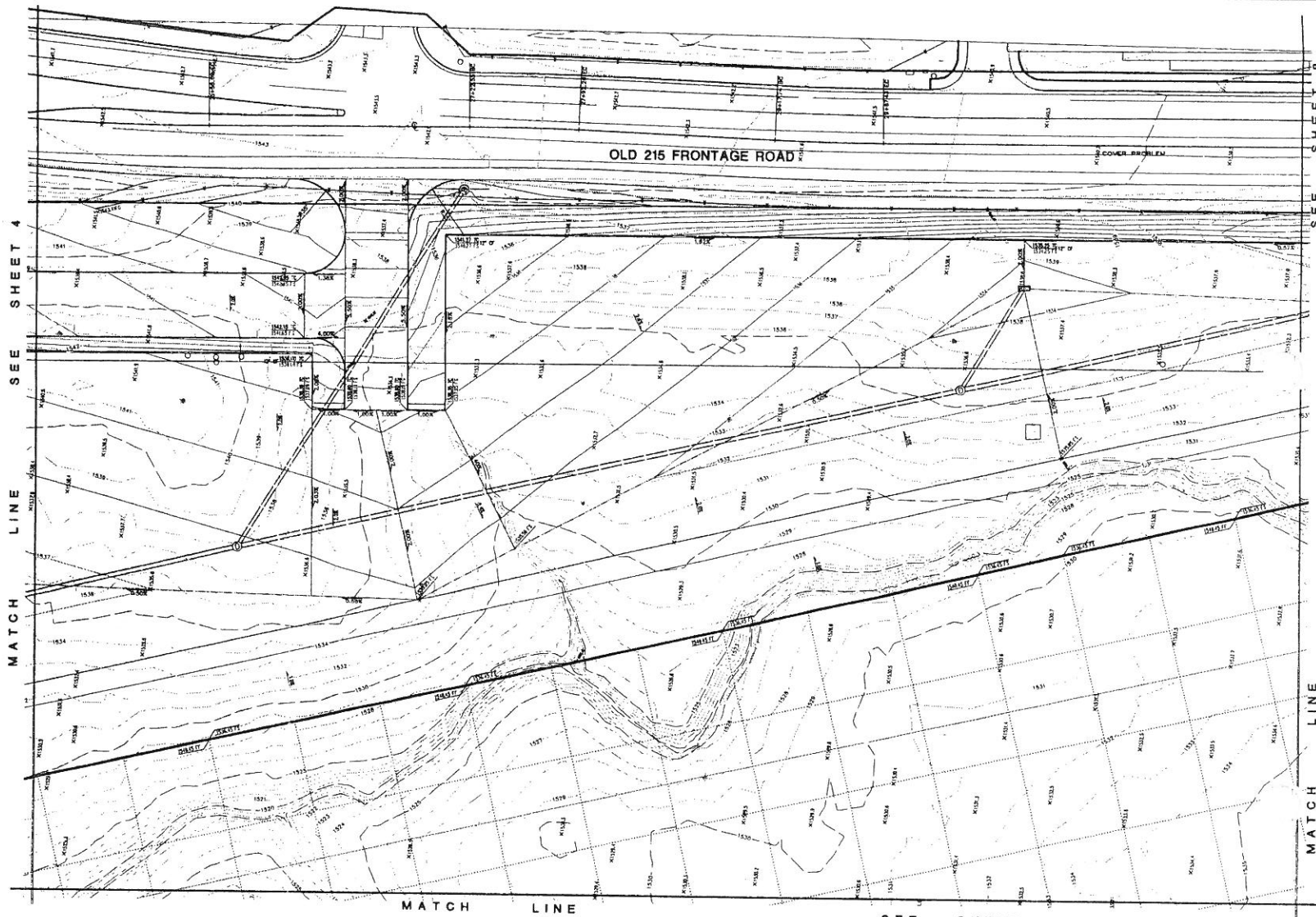
**Thomsen Engineering, Inc.**  
 Civil Engineering, Land Planning  
 & Land Development  
 18801 E. Oak Ave.  
 Redwood City, CA 94061  
 Tel: (650) 960-9900  
 Fax: (650) 960-8776

**BENCHMARK:**  
 SURVEY COUNTY BENCH MARK, 34-36, 1943  
 DISTANCE = 148.00 FEET - 34.00 FT  
 AT THE INTERSECTION OF POWER BUILDING  
 AND CLEARWAY (GRADE ELEVATION IN FEET WEST  
 OF POWER BUILDING). 64 FEET NORTH OF  
 THE WESTLY CORNER OF A 12' X 12' AS BUILT  
 CONCRETE PAD FOUND IN THE TOP OF  
 CONCRETE MATERIAL AND BARRIED IN 1985

PREPARED BY: HANS C. THOMSEN R.C.E. NO. 8244      DATE: \_\_\_\_\_  
 8/23/2016/06/30/2014 - FOR SUBMITTAL REVIEW

SCALE: 1" = 20' V. N/A.

FOR: PROPOUNTEE 215 LLC	W.O.	CITY FILE NO. _____
<b>MARCH JPA PRECISE GRADING PLAN FREEWAY BUSINESS CENTER PLAN B</b>		
WDID NO. _____	SHEET NO. <b>5</b>	OF 12 SHEETS



NOTE:  
 WORK CONTAINED WITHIN THESE PLANS SHALL NOT COMMENCE UNTIL AN ENCROACHMENT PERMIT AND/OR A GRADING PERMIT HAS BEEN ISSUED.

The printer retained during these plans is responsible for securing the accuracy and responsibility of the data herein. In the event of discrepancies arising after city approval or during construction, the printer engineer shall be responsible for determining an acceptable solution and making the plans be approved by the city.

MARK	DATE	REVISIONS	APPR. DATE

**MARCH JOINT POWERS AUTHORITY**  
 APPROVED BY: \_\_\_\_\_  
 DIRECTOR OF PUBLIC WORKS      DATE \_\_\_\_\_

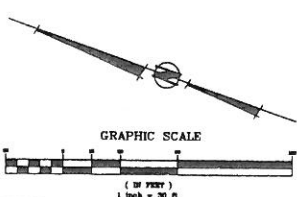
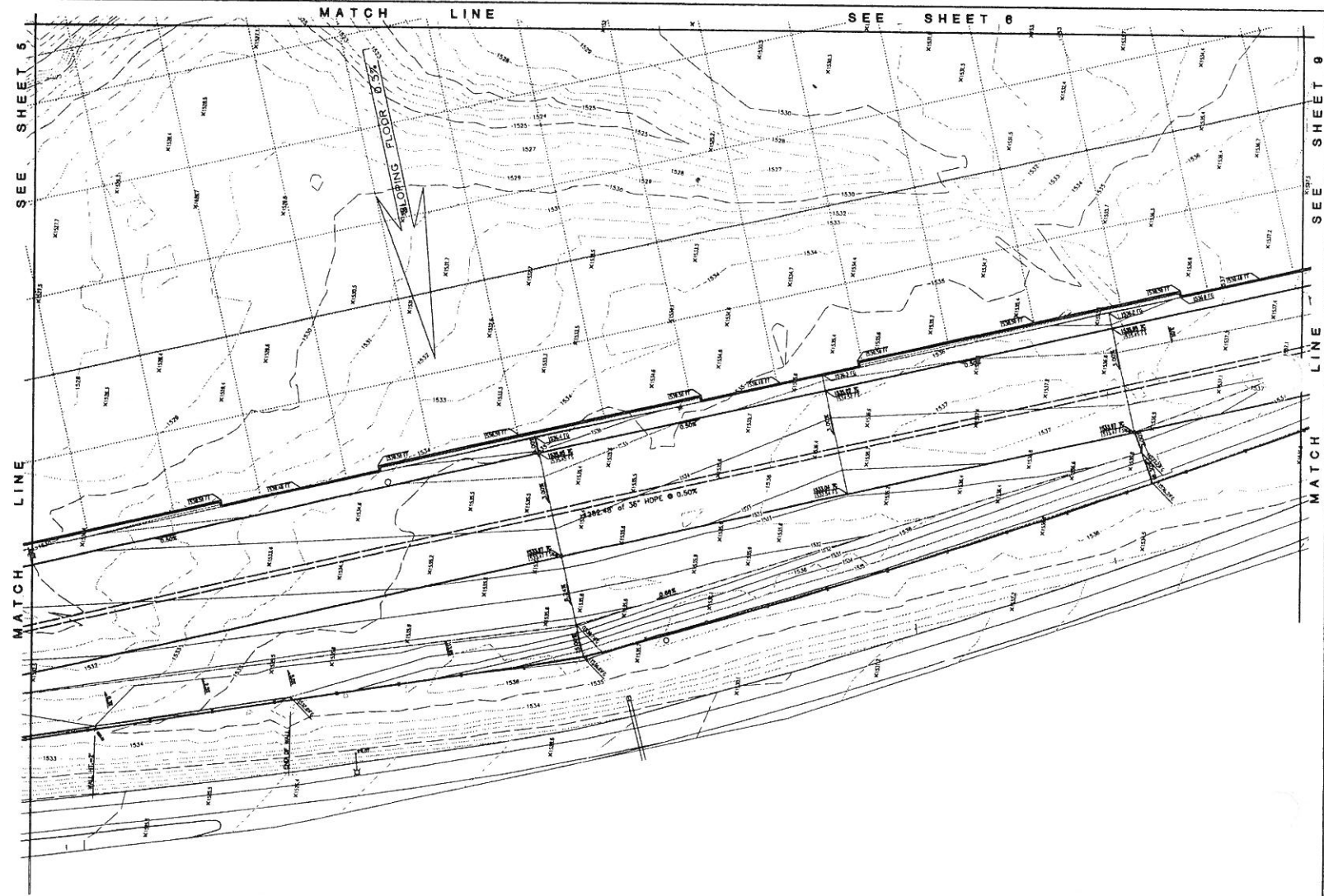


**Thomsen Engineering, Inc.**  
 CIVIL Engineering, Land Planning  
 & Land Surveying  
 18411 E. CALIF AVE.      TEL (925) 948-9300  
 REDDING, CA 96148      FAX (925) 948-2376

PREPARED BY: HANS C. THOMSEN R.C.E. NO. 8244      DATE \_\_\_\_\_  
 8233W\_06/30/2014 - FOR SUBMITTAL REVIEW

BENCHMARK:  
 MADRID COUNTY BENCH MARK, 30-36, 1943  
 ELEVATION = 1462.26, BENCH = 8000.00  
 ANY CHANGES SHOULD BE MADE WITHIN  
 15 DAYS OF PROJECT START DATE. THE DATE OF  
 THE SURVEY IS 02/11/2014. THE DATE OF  
 THE SURVEY IS 02/11/2014. THE DATE OF  
 CONSTRUCTION IS 02/11/2014.

WDID NO. _____	SHEET NO. <b>6</b>
<b>MARCH JPA</b>	
<b>PRECISE GRADING PLAN</b>	
<b>FREEWAY BUSINESS CENTER</b>	
<b>PLAN C</b>	
FOR: PROPRIETARY 215 LLC	W.O. _____
CITY FILE NO. _____	OF 12 SHEETS



NOTE:  
WORK CONTAINED WITHIN THESE PLANS SHALL NOT COMMENCE UNTIL AN ENCROACHMENT PERMIT AND/OR A GRADING PERMIT HAS BEEN ISSUED.

The private engineer displaying these plans is responsible for marking the corners and compatibility of the design herein. In the event of discrepancies arising after city approval or during construction, the private engineer shall be responsible for determining an acceptable solution and making the plans be returned by the city.

MARK BY	DATE	REVISIONS	APPR. DATE
ENGINEER			CITY

**MARCH JOINT POWERS AUTHORITY**  
APPROVED BY: \_\_\_\_\_  
DIRECTOR OF PUBLIC WORKS      DATE \_\_\_\_\_



**Thomsen Engineering, Inc.**  
Civil Engineering, Land Planning  
& Land Surveying  
18811 E. Galois Ave.  
Industry, CA 91748  
Tel. (929) 986-9960  
Fax (929) 986-9979

PREPARED BY: HANS C. THOMSEN R.C.E. NO. 8244      DATE \_\_\_\_\_  
8/13/08, 01/30/2014 - FOR SUBMITTAL REVIEW

**BENCHMARK:**  
IMPERIAL COUNTY BENCH 86-30, 1963  
ELEVATION = 144.06, 14.06 = 144.06  
AT THE INTERSECTION OF PIONEER AVENUE  
AND DELAWARE AVENUE, 100 FEET WEST  
OF PIONEER AVENUE, 15 FEET SOUTH OF  
THE INTERSECTION OF A 15' HIGH x 40' WIDE  
REARLAND, 6 INCHES DIA. SET IN THE TOP OF  
CONCRETE FOUNDATION, 100 WARD 8-20-1963

WDID NO. \_\_\_\_\_  
**MARCH JPA**  
**PRECISE GRADING PLAN**  
**FREWAY BUSINESS CENTER**

**PLAN D**  
FOR: PROPRIETARY 215 LLC      W.O.      CITY FILE NO. \_\_\_\_\_

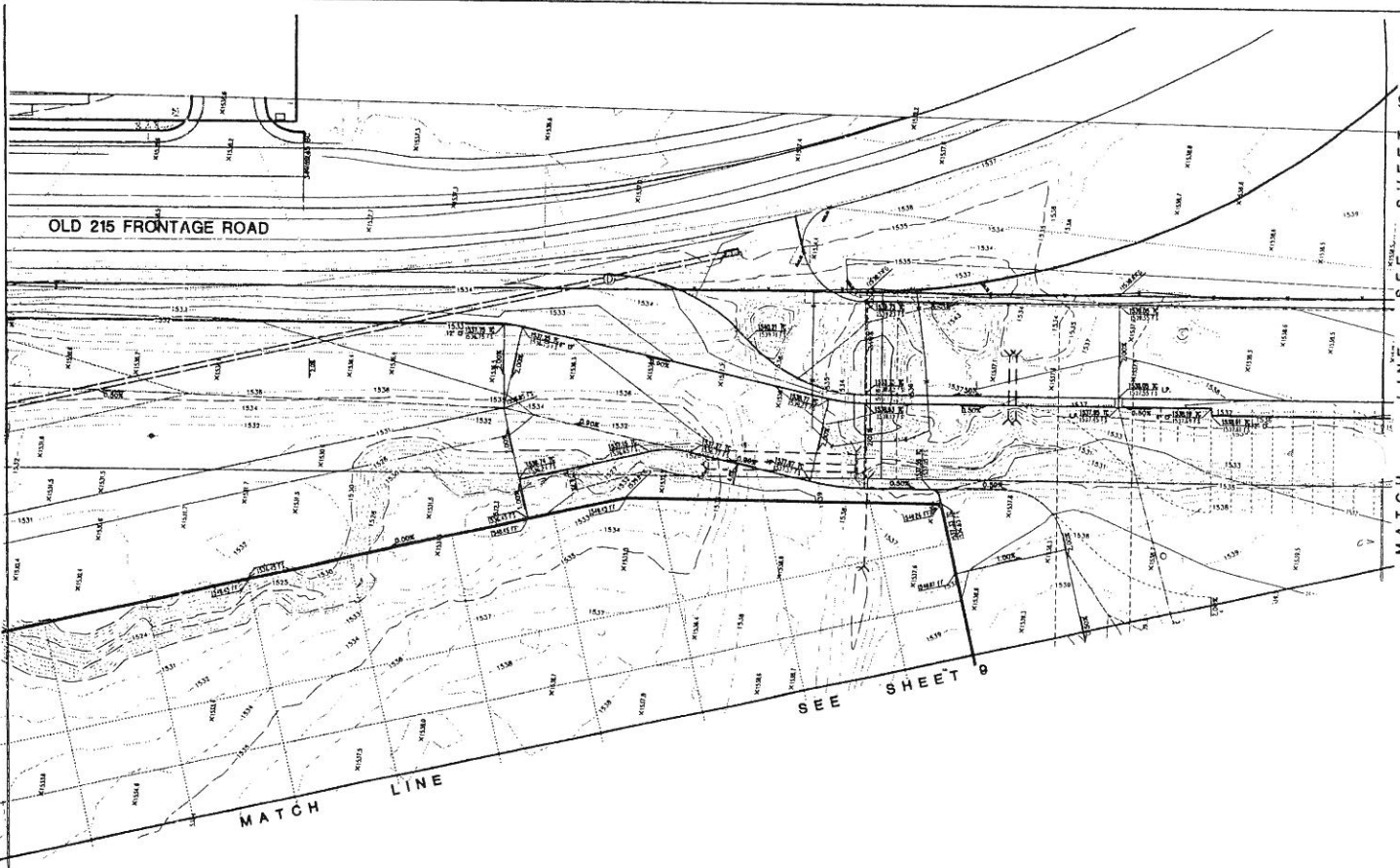
SHEET NO. **7**  
OF 12 SHEETS

SCALE: H. 1" = 30' V. N/A



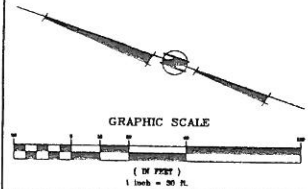
SEE SHEET 6  
MATCH LINE

SEE SHEET 10  
MATCH LINE



SEE SHEET 9

MATCH LINE



NOTE:  
WORK CONTAINED WITHIN THESE PLANS SHALL NOT COMMENCE UNTIL AN ENCROACHMENT PERMIT AND/OR A GRADING PERMIT HAS BEEN ISSUED.

The private engineer hereby being placed is responsible for securing the accuracy and availability of the design herein. In the event of discrepancies arising after the approval of design construction, the private engineer shall be responsible for determining an acceptable solution and making the plans for approval by the city.

MARK	BY	DATE	REVISIONS	APPR.	DATE	CITY

**MARCH JOINT POWERS AUTHORITY**  
APPROVED BY: \_\_\_\_\_  
DIRECTOR OF PUBLIC WORKS      DATE \_\_\_\_\_



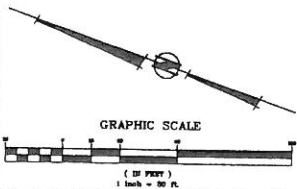
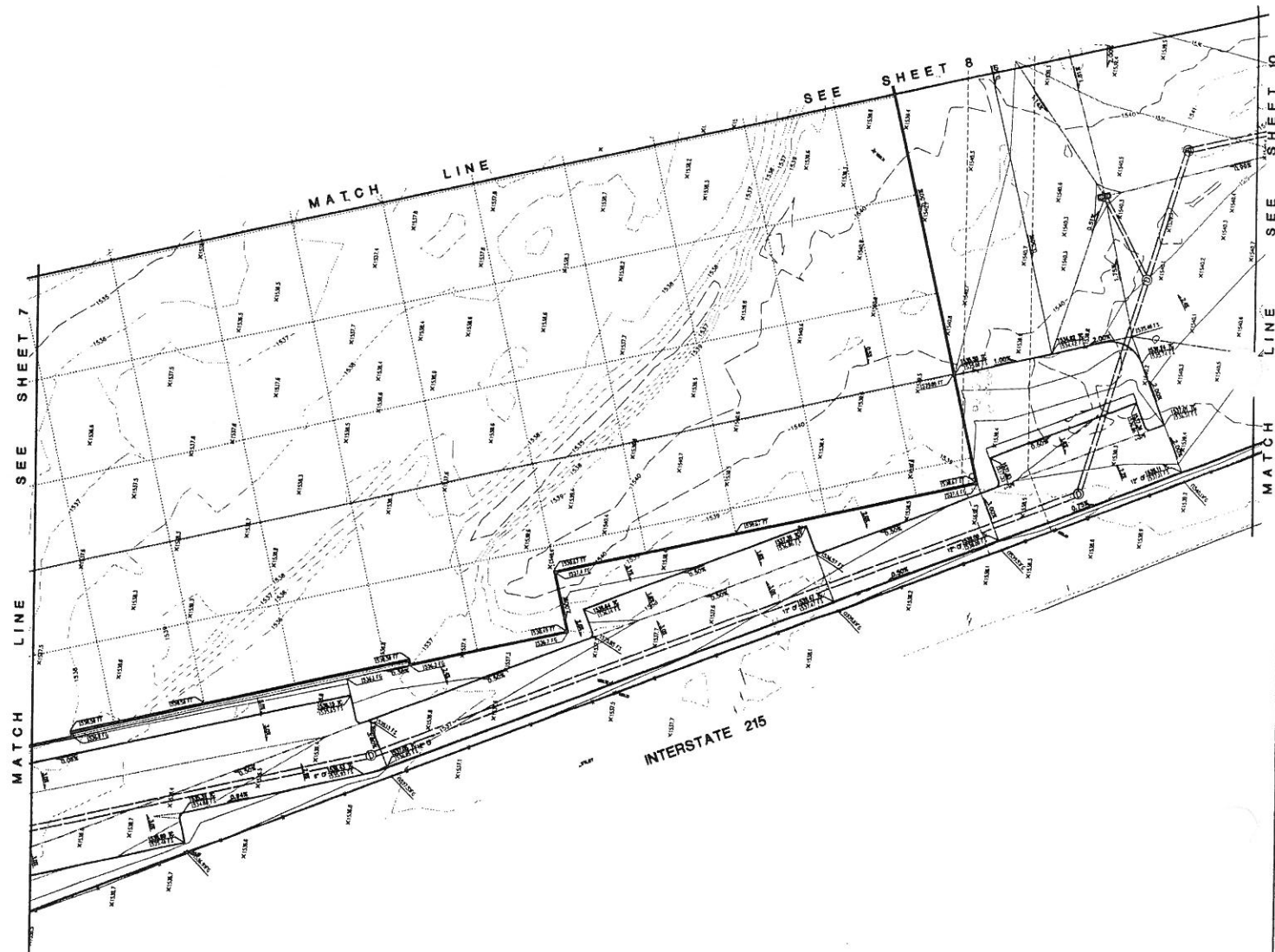
**Thomsen Engineering, Inc.**  
Civil Engineering, Land Planning  
& Land Surveying  
15811 E. Gale Ave.      Tel: (925) 986-0960  
Redwood City, CA 94068      Fax: (925) 986-2796

PREPARED BY: HANS C. THOMSEN R.C.E. NO. 8244      DATE \_\_\_\_\_  
82130\_08/30/2014 - FOR SUBMITTAL REVIEW

BENCHMARK:  
MARCH COUNTY MARCH 2008, W-26, 1461  
ELEVATION = 1462.00, 1462.00 ±  
41 NE INTERSECTION OF PIONEER BOULEVARD  
AND DELANEO DRIVE (ELEV. 1462.00) WEST  
OF PIONEER BOULEVARD, 1/4 SECTION 28  
OF TOWNSHIP 20S OF R. 11E, RANGE 14E, T20S  
R11E, S28, 1/4 SECTION 28, 1/4 SECTION 28, 1/4 SECTION 28

WDID NO. _____	SHEET NO. <b>8</b>
<b>MARCH JPA</b>	
<b>PRECISE GRADING PLAN</b>	
<b>FREEWAY BUSINESS CENTER</b>	
<b>PLAN E</b>	
FOR: PROPRICIENCY 215 LLC	CITY FILE NO. _____

OF 12 SHEETS



NOTE:  
 WORK CONTAINED WITHIN THESE PLANS SHALL NOT COMMENCE UNTIL AN ENCROACHMENT PERMIT AND/OR A GRADING PERMIT HAS BEEN ISSUED.

The printer neither displays these plans is responsible for securing the accuracy and completeness of the data hereon. In the event of discrepancies arising after city approval or during construction, the printer neither shall be responsible for determining an acceptable solution and making the plans be approved by the city.

MARK BY	DATE	REVISIONS	APPR. DATE

MARCH JOINT POWERS AUTHORITY

APPROVED BY: \_\_\_\_\_  
 DIRECTOR OF PUBLIC WORKS      DATE \_\_\_\_\_



Thomsen Engineering, Inc.  
 Civil Engineering, Land Planning  
 8 Land Parkway  
 3405 E. Maple Ave.  
 Industry, CA 91746  
 Tel: (925) 986-8900  
 Fax: (925) 986-8978

PREPARED BY: HANS C. THOMSEN R.C.E. NO. 8244      DATE \_\_\_\_\_  
 02/28/20/3/30/2014 - FOR SUBMITTAL REVIEW

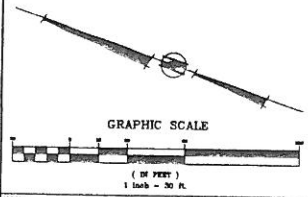
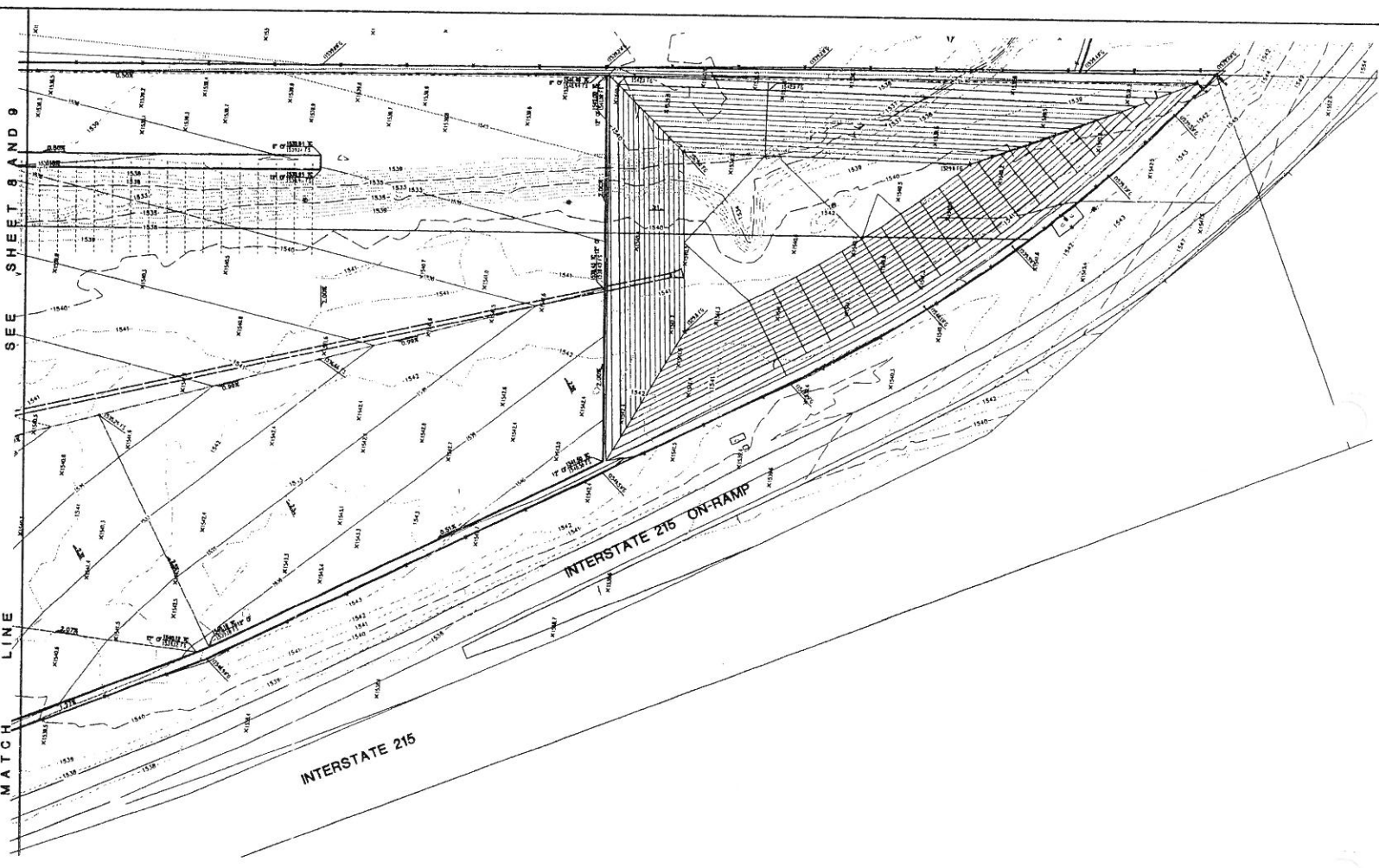
BENCHMARK:  
 INDEPENDENT COUNTY GRADE BENCH - 10.145  
 ELEVATION = 104.274, DATUM = MDD 28  
 AT THE INTERSECTION OF NORTH BOLLINGER  
 AND CLEARVIEW AVENUE (EAST) IN WEST PART  
 OF TOWNSHIP 10 NORTH, 13 WEST RANGE OF  
 THE NORTHERLY END OF A 22 INCH x 44 INCH  
 METERS, A BRASS ROD SET IN THE TOP OF A  
 CONCRETE HEADWALL AND MARKED B-30 THIS

SCALE: H-1" = 30' V- N/A

WDID NO. -----		SHEET NO.
MARCH JPA		9
PRECISE GRADING PLAN		
FREWAY BUSINESS CENTER		
PLAN F		OF 12 SHTS
FOR: PROPRIETARY 215 LLC	W.O.	CITY FILE NO. ----

SEE SHEET 8 AND 9

MATCH LINE



**NOTE:**  
 WORK CONTAINED WITHIN THESE PLANS SHALL NOT COMMENCE UNTIL AN ENCROACHMENT PERMIT AND/OR A GRADING PERMIT HAS BEEN ISSUED.

The private engineer applying these plans is responsible for securing the necessary and complete information in the event of discrepancies arising after the approval of these construction. The private engineer shall be responsible for determining an acceptable solution and making the plans be approved by the City.

MARK BY	DATE	REVISIONS	APPR.	DATE
ENGINEER				

**MARCH JOINT POWERS AUTHORITY**

APPROVED BY: \_\_\_\_\_

DIRECTOR OF PUBLIC WORKS

DATE \_\_\_\_\_



**Thomsen Engineering, Inc.**  
 Civil Engineering, Land Planning & Land Surveying  
 18411 R. Dale Ave., Industry, CA 91748  
 Tel: (925) 998-2900 Fax: (925) 998-2578

PREPARED BY: HANS C. THOMSEN R.C.E. NO. 8244 DATE \_\_\_\_\_

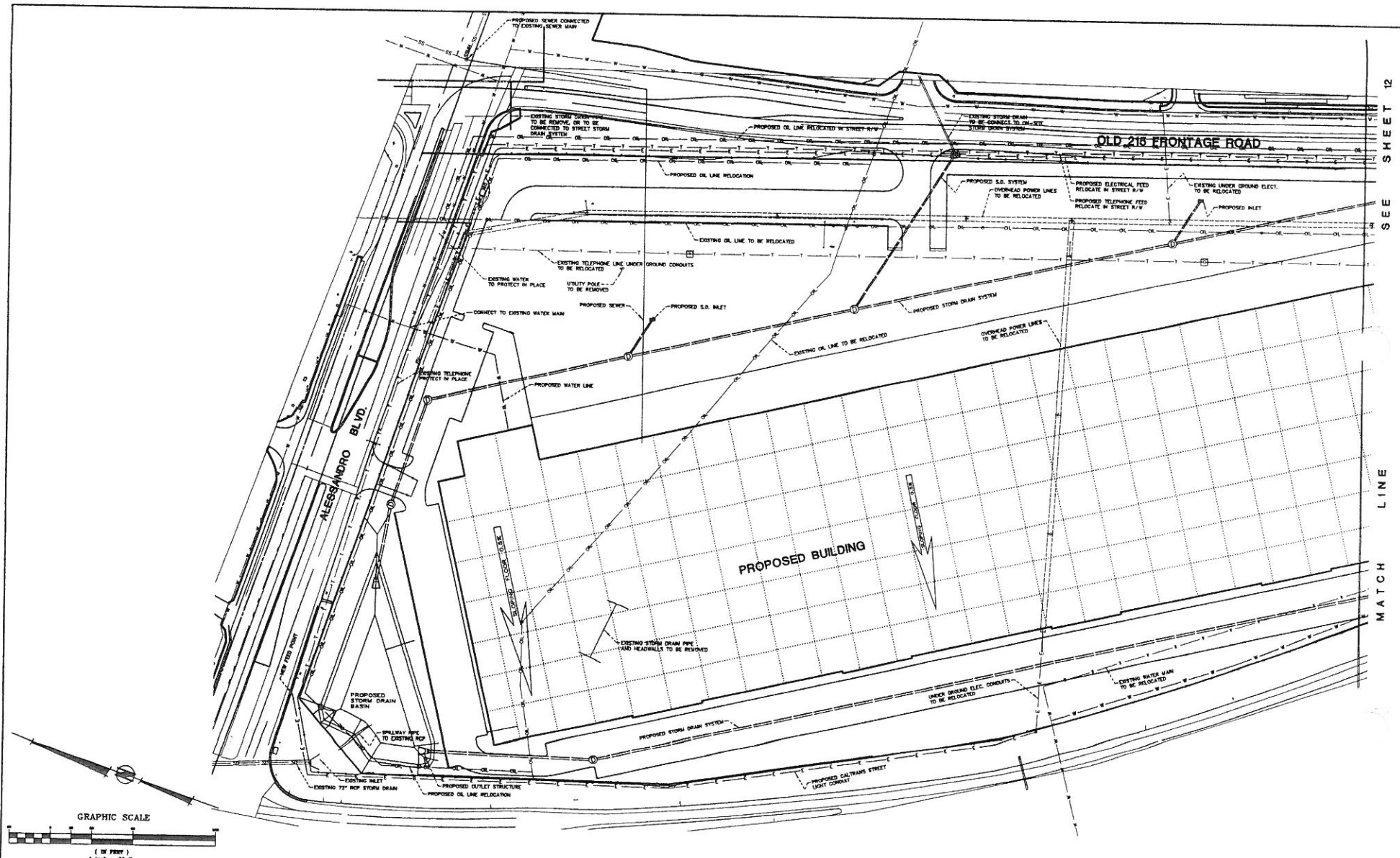
82530 06/30/2014 - FOR SUBMITTAL REVIEW

**BENCHMARK:**  
 BENCHM. CHERRY BRICK PILE 30-30 1983  
 ELEVATION = 144.26 ± 0.04 - POINT 20  
 AT THE INTERSECTION OF POWER BRIDGE AND  
 AND CLEARANCE BRIDGE (ELEV. 141 FEET 2001)  
 OF POWER BRIDGE. ALL POINTS BEING OF  
 THE NORTHERLY END OF A 15 INCH x 18 CITY  
 REINFORCED CONCRETE PILE SET IN 18" TOP OF  
 CONCRETE HEADWALL AND BARRIED 10-20 1983

SCALE: 1" = 30' V. N/A

WDID NO. -----	SHEET NO. 10
MARCH JPA	of 12 SHEETS
<b>PRECISE GRADING PLAN</b>	
<b>FREEWAY BUSINESS CENTER</b>	
PLAN G	
FOR: PROPRIETARY 215 LLC	CITY FILE NO. -----

SHEET 12  
SEE  
MATCH LINE



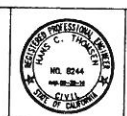
GRAPHIC SCALE  
1 inch = 60 ft.

NOTE:  
WORK CONTAINED WITHIN THESE PLANS SHALL NOT COMMENCE UNTIL AN ENCROACHMENT PERMIT AND/OR A GRADING PERMIT HAS BEEN ISSUED.

The planee engineer accepts these plans as responsible for carrying the accuracy and completeness of the design herein. In the event of discrepancies arising after any approval or during construction, the planee engineer shall be responsible for determining an acceptable solution and making the plans be approved by the City.

MARK	BY	DATE	REVISIONS	APPR.	DATE	QTY

**MARCH JOINT POWERS AUTHORITY**  
APPROVED BY: \_\_\_\_\_  
DIRECTOR OF PUBLIC WORKS  
DATE: \_\_\_\_\_



**Thomsen Engineering, Inc.**  
Civil Engineering, Land Planning  
8 Land Surveying  
3865 E. 94th Ave.  
Inglewood, CA 91748  
Tel (805) 866-9980  
Fax (805) 866-8976

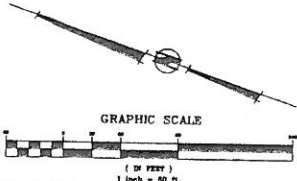
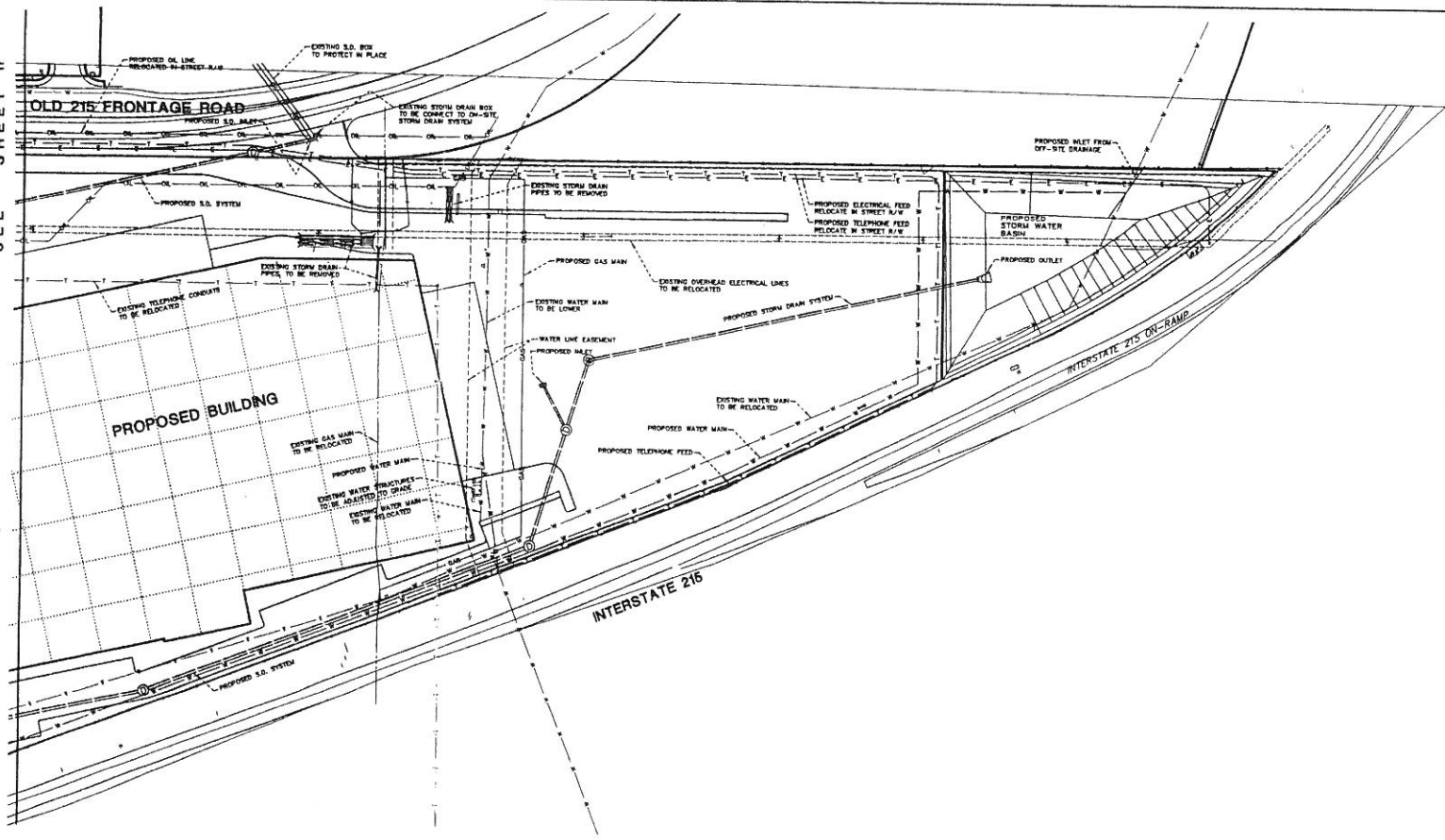
BENCHMARK:  
SAN DIEGO COUNTY BEARING 06-31-1983  
DISTANCE - 148.00 FT. ± TO  
AT THE INTERSECTION OF POWER BOULEVARD  
AND CLEARVIEW AVENUE IN THE WEST  
END OF POWER BOULEVARD. ALL FEET MEASUREMENTS  
TO THE NORTHERLY END OF A 12 INCH x 12 INCH  
PLUMBING. A BENCH MARK SET IN THE TOP OF  
CONCRETE FOUNDATION AND DATED 9-10-1983

WDID NO. -----  
MARCH JPA  
PRECISE GRADING PLAN  
FREEWAY BUSINESS CENTER  
UTILITIES - NORTH  
FOR: PROFICIENCY 215 LLC  
CITY FILE NO. -----  
SHEET NO. 11  
OF 12 SHEETS

PREPARED BY: HANS C. THOMSEN R.C.E. NO. 8244  
DATE: \_\_\_\_\_  
82230-06/30/2014 - FOR SUBMITTAL REVIEW

SCALE: 1" = 60' ± N/A

SHEET 11  
SEE  
LINE  
MATCH



**NOTE:**  
WORK CONTAINED WITHIN THESE PLANS SHALL NOT COMMENCE UNTIL AN ENCROACHMENT PERMIT AND/OR A GRADING PERMIT HAS BEEN ISSUED.

The private engineer drawing these plans is responsible for checking the accuracy and responsibility of the data furnished. In the event of discrepancies arising after city approval or during construction, the private engineer shall be responsible for determining or completing suitable and making the plans for record by the city.

MARK BY	DATE	REVISIONS	APPR. DATE
ENGINEER			CITY

**MARCH JOINT POWERS AUTHORITY**  
APPROVED BY: \_\_\_\_\_  
DIRECTOR OF PUBLIC WORKS      DATE \_\_\_\_\_



**Thomsen Engineering, Inc.**  
Civil Engineering, Land Planning  
& Land Surveying  
18411 E. Gale Ave.      Tel: (925) 955-2300  
Industry, CA 94748      Fax: (925) 955-2379

PREPARED BY: HANS C. THOMSEN      R.C.E. NO. 8244      DATE \_\_\_\_\_  
8133N\_09/30/2014 - FOR SUBMITTAL REVIEW

**BENCHMARK:**  
MARCH COUNTY BENCH 001, U.S. 36, 1983  
ELEVATION = 443.05 ±      8006.75  
AT THE INTERSECTION OF POWER AND  
AND RELEASED UNDER PERMIT NO. 107  
OF POWER AND RELEASED, 45 FEET SOUTH OF  
THE INTERSECTION OF P.A. 15, 1961 ± 80 FEET  
ELEVATION: 4 BENCH SET SET IN THE TOP OF  
CONCRETE FOUNDATION, AND DATED 10-20-1983

WDID NO. \_\_\_\_\_  
MARCH JPA  
**PRECISE GRADING PLAN**  
**FREEWAY BUSINESS CENTER**  
UTILITIES - SOUTH

SCALE: H. 1" = 80' V. N.A.  
FOR: PROFICIENCY 215 LLC      W.O.      CITY FILE NO. \_\_\_\_\_  
SHEET NO. 12      OF 12 SHOTS

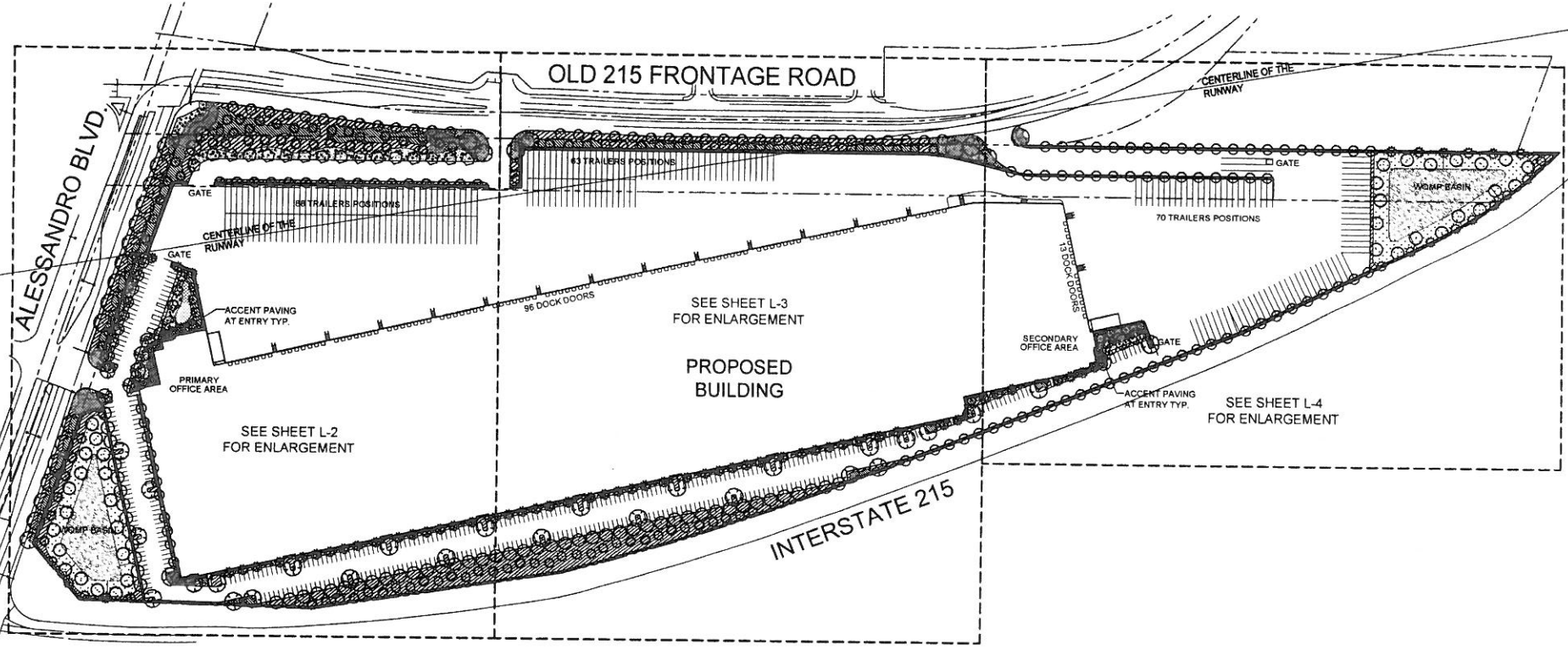
**TREE LEGEND**

SIZE	BOTANICAL NAME / COMMON NAME	CONT.	WATER USE	SHRUB	SIZE
24" W x 18" D	Cercis x Desert Museum / Thornless Palo Verde	24" W	Low	18	
18" W x 15" D	Eucalyptus alternata / Rose / Red Tornado	15" W	Low	18	
18" W x 15" D	Plusia alberta / Afghan Pine / ACE - 34" BOX - WATER USE - MILD CL - LOW	15" W	Low	100	
18" W x 15" D	Plusia alberta / Afghan Pine / ACE - 24" BOX - WATER USE - MILD CL - LOW	24" W	Low	92	
24" W x 18" D	Platanus orientalis / Chinese Plane	24" W	Low	83	
15" W x 15" D	Platanus racemosa / California Sycamore	15" W	Med	38	
24" W x 18" D	Prosopis juliflora / Chinese Mesquite	24" W	Low	11	
24" W x 18" D	Rhus laevis / African Senece	24" W	Low	55	
24" W x 18" D	v. Chilensis / California Palo Verde / Pink Dawn / Pink Dawn Chilensis	24" W	Low	38	

**CONCEPT PLANT SCHEDULE**

FOUNDATIONS PLANTING - 12" W x 12" D	12" W x 12" D	268
LARGE ACCENT SHRUBS - 5' GAL	LOW WATER	47
LARGE SCALE FOUNDATION SHRUBS - 5' GAL	LOW WATER	1,381
SMALL SCALE FOUNDATION PLANTING - 5' GAL	LOW WATER	237
SMALLER ACCENT SHRUBS - 5' GAL	LOW WATER	172

SHRUB PALETTE - MEDIUM WATER USE	12,734 W
WORMY BUSH PLANTING PALETTE	4,730 W
SHRUB PALETTE - LOW WATER USE	31,074 W
GROUND COVER PALETTE - LOW WATER USE - 1 GAL	167,627 W
GROUND COVER PALETTE - MEDIUM WATER USE - 1 GAL	16,091 W
GRASS - HIGH WATER USE	2,336 W



**Proficiency 2145 LLC**  
 1777 SAN VICENTE BLVD., SUITE 300  
 LOS ANGELES, CA 90049  
 TEL: (310) 395-8000  
 www.proficiency2145.com

SHEET TITLE: PRELIMINARY LANDSCAPE  
 PROJECT DEVELOPER: FREEMAN BUSINESS CENTER  
 0000 ALESSANDRO BLVD.  
 MARCH, JPA, CA

DRAWN BY: DATE  
 BFK 12/04/14  
 JOB NO.:  
 ROAD005  
 SHEET NO.:  
**L-1**  
 1 of 4 SHEETS

ALESSANDRO BLVD.

OLD 215 FRONTAGE ROAD

CENTERLINE OF THE RUNWAY

PROPOSED BUILDING

INTERSTATE 215

WOMP BASIN

PRIMARY OFFICE SPACE

ACCENT PAVING AT ENTRY TYP.

96 DOCK DOORS

86 TRAILERS POSITIONS

GATE

GATE

TREE LEGEND

TREES	BOTANICAL NAME / COMMON NAME	COHT	WALCOOL	QTY
	Cardinalis / Desert Mourner / Thornless Palo Verde	24" to 30"	Low	18
	Cardinalis / Desert Mourner / Thornless Palo Verde	48" to 72"	Low	15
	Fraxinus velutina / Rose / Red Ironbark	18" to 24"	Low	95
	Prosopis juliflora / Algarin Pine	15" to 18"	Low	100
	Prosopis juliflora / Algarin Pine	24" to 30"	Low	82
	Prosopis juliflora / Algarin Pine	24" to 30"	Low	53
	Prosopis juliflora / Algarin Pine	15" to 18"	Med	38
	Prosopis juliflora / Algarin Pine	24" to 30"	Low	11
	Rhus typhina / African Sycamore	24" to 30"	Low	55
	Chrysomela trichostema / Pink Dawn / Pink Dawn Orange	24" to 30"	Low	35

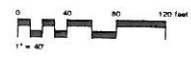
CONCEPT PLANTING SCHEDULE

	FOUNDATION PLANTING / HEDGE SCREEN - 5 GAL - MED WATER <i>Banksia integrifolia</i> / Japanese Garden Elm / <i>Prunus Serrulata</i> / <i>Prunus Serrulata</i> - Space 30' o.c. <i>Leucodermis</i> / <i>Leucodermis</i> - Space 5' o.c. <i>Nandina domestica</i> / Heavenly Bamboo - Space 3' o.c. <i>Podocarpus neriifolius</i> / Silver Choke / <i>Podocarpus</i> - Space 5' o.c. <i>Podocarpus neriifolius</i> / Silver Choke / <i>Podocarpus</i> - Space 5' o.c. <i>Podocarpus neriifolius</i> / Silver Choke / <i>Podocarpus</i> - Space 5' o.c. <i>Podocarpus neriifolius</i> / Silver Choke / <i>Podocarpus</i> - Space 5' o.c. <i>Podocarpus neriifolius</i> / Silver Choke / <i>Podocarpus</i> - Space 5' o.c.	268
	LARGE ACCENT SHRUBS - 5 GAL - LOW WATER <i>Agave americana</i> / Spanish Palm - Space 6' o.c. <i>Agave americana</i> / Spanish Palm - Space 6' o.c. <i>Agave americana</i> / Spanish Palm - Space 6' o.c.	47
	LARGE SCALE FOUNDATION SHRUBS - 3 GAL - LOW WATER <i>Arbutus unedo</i> / Strawberry Tree Shrub - Space 3' o.c. <i>Calliandra cuneata</i> / Lemon Bottlebrush Shrub - Space 3' o.c. <i>Calliandra cuneata</i> / Lemon Bottlebrush Shrub - Space 3' o.c. <i>Calliandra cuneata</i> / Lemon Bottlebrush Shrub - Space 3' o.c. <i>Calliandra cuneata</i> / Lemon Bottlebrush Shrub - Space 3' o.c. <i>Calliandra cuneata</i> / Lemon Bottlebrush Shrub - Space 3' o.c.	1,331
	SMALL SCALE FOUNDATION PLANTING - 5 GAL - LOW WATER <i>Arbutus unedo</i> / Strawberry Tree Shrub - Space 3' o.c. <i>Calliandra cuneata</i> / Lemon Bottlebrush Shrub - Space 3' o.c. <i>Calliandra cuneata</i> / Lemon Bottlebrush Shrub - Space 3' o.c. <i>Calliandra cuneata</i> / Lemon Bottlebrush Shrub - Space 3' o.c. <i>Calliandra cuneata</i> / Lemon Bottlebrush Shrub - Space 3' o.c. <i>Calliandra cuneata</i> / Lemon Bottlebrush Shrub - Space 3' o.c.	237
	SMALLER ACCENT SHRUBS - 5 GAL - LOW WATER <i>Agave americana</i> / Spanish Palm - Space 6' o.c. <i>Agave americana</i> / Spanish Palm - Space 6' o.c. <i>Agave americana</i> / Spanish Palm - Space 6' o.c. <i>Agave americana</i> / Spanish Palm - Space 6' o.c. <i>Agave americana</i> / Spanish Palm - Space 6' o.c. <i>Agave americana</i> / Spanish Palm - Space 6' o.c.	172
	SHRUB PALETTE - MEDIUM WATER USE <i>Banksia integrifolia</i> / Japanese Garden Elm / <i>Prunus Serrulata</i> / <i>Prunus Serrulata</i> - Space 30' o.c. <i>Leucodermis</i> / <i>Leucodermis</i> - Space 5' o.c. <i>Nandina domestica</i> / Heavenly Bamboo - Space 3' o.c. <i>Podocarpus neriifolius</i> / Silver Choke / <i>Podocarpus</i> - Space 5' o.c. <i>Podocarpus neriifolius</i> / Silver Choke / <i>Podocarpus</i> - Space 5' o.c. <i>Podocarpus neriifolius</i> / Silver Choke / <i>Podocarpus</i> - Space 5' o.c.	12,734 u
	WOMP BASIN PLANTING PALETTE <i>Agave americana</i> / Spanish Palm - Space 6' o.c. <i>Agave americana</i> / Spanish Palm - Space 6' o.c. <i>Agave americana</i> / Spanish Palm - Space 6' o.c. <i>Agave americana</i> / Spanish Palm - Space 6' o.c. <i>Agave americana</i> / Spanish Palm - Space 6' o.c. <i>Agave americana</i> / Spanish Palm - Space 6' o.c.	14,730 u
	SHRUB PALETTE - LOW WATER USE <i>Agave americana</i> / Spanish Palm - Space 6' o.c. <i>Agave americana</i> / Spanish Palm - Space 6' o.c. <i>Agave americana</i> / Spanish Palm - Space 6' o.c. <i>Agave americana</i> / Spanish Palm - Space 6' o.c. <i>Agave americana</i> / Spanish Palm - Space 6' o.c. <i>Agave americana</i> / Spanish Palm - Space 6' o.c.	31,074 u
	GROUND COVER PALETTE - LOW WATER USE <i>Banksia integrifolia</i> / Japanese Garden Elm / <i>Prunus Serrulata</i> / <i>Prunus Serrulata</i> - Space 30' o.c. <i>Leucodermis</i> / <i>Leucodermis</i> - Space 5' o.c. <i>Nandina domestica</i> / Heavenly Bamboo - Space 3' o.c. <i>Podocarpus neriifolius</i> / Silver Choke / <i>Podocarpus</i> - Space 5' o.c. <i>Podocarpus neriifolius</i> / Silver Choke / <i>Podocarpus</i> - Space 5' o.c. <i>Podocarpus neriifolius</i> / Silver Choke / <i>Podocarpus</i> - Space 5' o.c.	1,173,427 u
	GROUND COVER PALETTE - MEDIUM WATER USE <i>Banksia integrifolia</i> / Japanese Garden Elm / <i>Prunus Serrulata</i> / <i>Prunus Serrulata</i> - Space 30' o.c. <i>Leucodermis</i> / <i>Leucodermis</i> - Space 5' o.c. <i>Nandina domestica</i> / Heavenly Bamboo - Space 3' o.c. <i>Podocarpus neriifolius</i> / Silver Choke / <i>Podocarpus</i> - Space 5' o.c. <i>Podocarpus neriifolius</i> / Silver Choke / <i>Podocarpus</i> - Space 5' o.c. <i>Podocarpus neriifolius</i> / Silver Choke / <i>Podocarpus</i> - Space 5' o.c.	16,081 u
	GRASS - HIGH WATER USE	2,338 u

NOTE:  
 1. ALL TREES WITHIN 5' OF HARDSCAPE SHALL BE IN A SHALLOW LIME PLANTER UNLESS NOT ALLOWED BY CITY ORDINANCE. ALL TREES SHALL BE PLANTED IN THE MIDDLE OF THE ROW AND EXTEND IN BOTH DIRECTIONS FOR A TOTAL OF 10'.

REFERENCE NOTES SCHEDULE

SYMBOL	DESCRIPTION	QTY
1	3" THICK x 30' DECORATIVE GRAVEL (NO LIVE GOLF) WITH RANDOM ACCENT BOLDERS (DESERT SAND) - OVER WEP	18,450 sq
2	6" x 6" CONCRETE MONUMENTS TO SEPARATE SHRUB AREAS FROM GRAVEL	
3	NEVER ROCK COBBLE 4" - 12"	36,387 sq



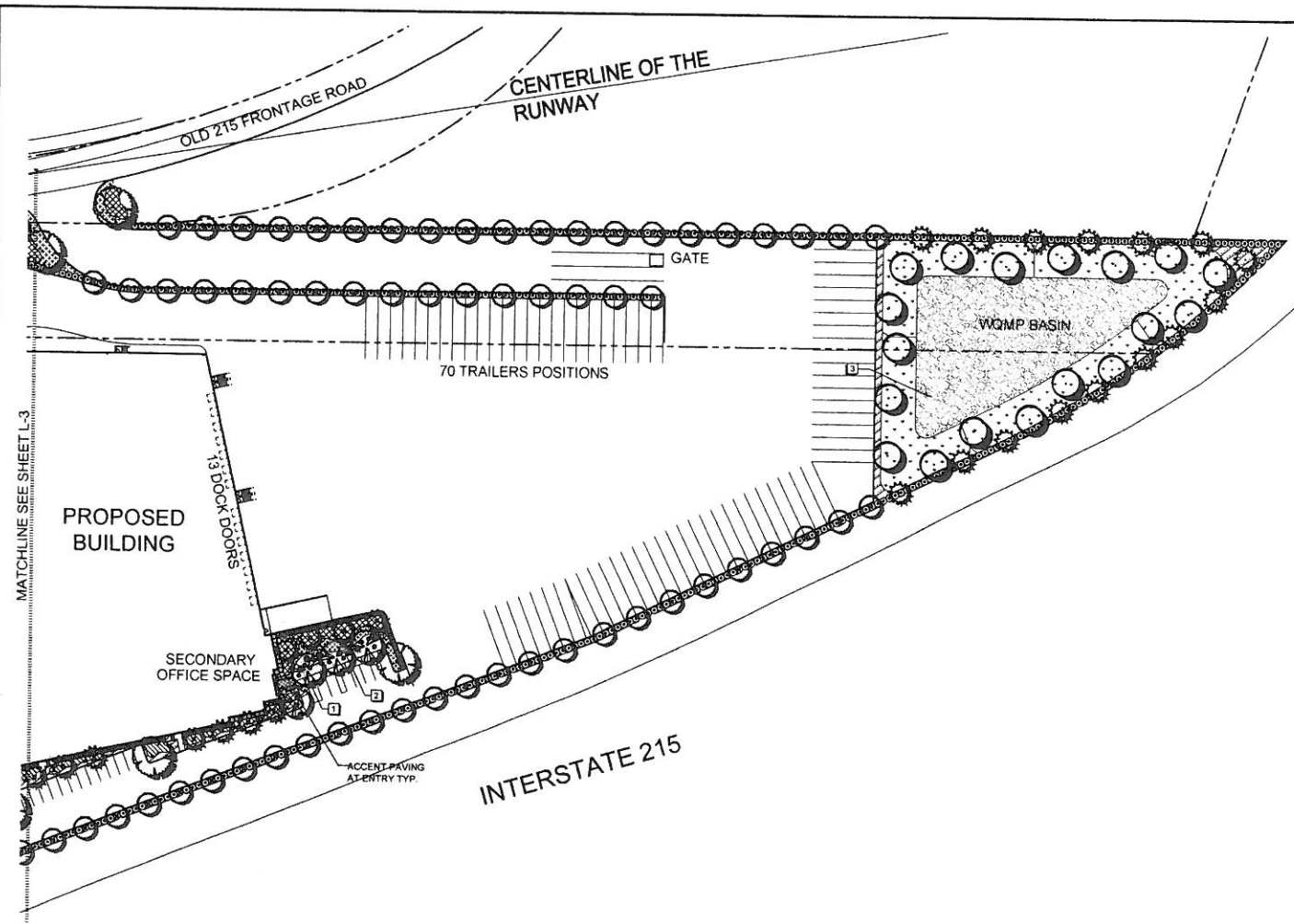
PROFICIENCY 215 LLC  
 11777 VAN VENTURE BLVD, SUITE 101  
 LOS ANGELES, CA 90049  
 TEL: (310) 375-9000

PRELIMINARY LANDSCAPE  
 FREEMWAY BUSINESS CENTER  
 0000 ALESSANDRO BLVD.  
 MARCH JPA, CA

DATE: 12/24/14  
 JOB NO: RG00055  
 SHEET NO: L-2  
 OF 4 SHEETS







### TREE LEGEND

TREE	BOTANICAL NAME / COMMON NAME	DBHT	WALCOL III	QTY
	Cercidion x Desert Museum / Thornless Palo Verde	24" to 30"	Low	18
	Cercidion x Desert Museum / Thornless Palo Verde	48" to 60"	Low	16
	Condalia leucosticta / Rose / Red barberton	15' to 20'	Low	96
	Pinus wislizeni / Algarine Pine SEE 24" BOX - WATER USE: WALCOL - LOW	15' to 20'	Low	100
	Pinus wislizeni / Algarine Pine SIZE - 24" BOX - WATER USE: WALCOL - LOW	24" to 30"	Low	92
	Platanus obtusifolia / Chinese Plane	24" to 30"	Low	53
	Platanus mexicana / California Sycamore	15' to 20'	Med	36
	Prosopis juliflora / Chinese Mesquite	24" to 30"	Low	11
	Rhus typhina / African Sycamore	24" to 30"	Low	55
	Rhus typhina / African Sycamore	24" to 30"	Low	35

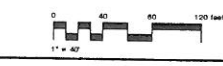
### CONCEPT PLANT SCHEDULE

	<b>FOUNDATION PLANTING / HEDGE SCREEN - 5 GAL. AUTO WATER</b> Banksia integrifolia / Australian Banksia / Lemon Biscuit Elmwood - Spacing 30' x 6" Livistonia laurifolia / Palm Fern - Spacing 3' x 6" Metrosideros excelsa / Huckleberry Bush - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 5' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6"	150
	<b>LARGE ACCENT SHRUBS - 5 GAL. LOW WATER</b> Agave americana / Century Plant - Spacing 6' x 6" Atriplex confertifolia / Shrub Salt-Tolerant - Spacing 4' x 6" Daphne genkwa / Grey Daphne - Spacing 5' x 6"	47
	<b>LARGE SCALE FOUNDATION SHRUBS - 5 GAL. LOW WATER</b> Adiantum species / Fern - Spacing 3' x 6" Callistemon citrinus / Lemon Bottlebrush - Spacing 3' x 6" Dioscorea oppositifolia / Yucca - Spacing 3' x 6" Dioscorea oppositifolia / Yucca - Spacing 3' x 6" Heterostemma arborescens / Yucca - Spacing 4' x 6" Lonicera caerulea / California Laurel - Spacing 5' x 6"	1,331
	<b>SMALL SCALE FOUNDATION PLANTING - 5 GAL. LOW WATER</b> Banksia integrifolia / Australian Banksia / Lemon Biscuit Elmwood - Spacing 3' x 6" Callistemon citrinus / Lemon Bottlebrush - Spacing 3' x 6" Dioscorea oppositifolia / Yucca - Spacing 3' x 6" Heterostemma arborescens / Yucca - Spacing 4' x 6" Lonicera caerulea / California Laurel - Spacing 5' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6"	237
	<b>SMALLER ACCENT SHRUBS - 5 GAL. LOW WATER</b> Agave americana / Century Plant - Spacing 6' x 6" Atriplex confertifolia / Shrub Salt-Tolerant - Spacing 4' x 6" Daphne genkwa / Grey Daphne - Spacing 5' x 6" Lonicera caerulea / California Laurel - Spacing 5' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6"	172
	<b>SHRUB PALETTE - MEDIUM WATER USE</b> Banksia integrifolia / Australian Banksia / Lemon Biscuit Elmwood - Spacing 3' x 6" Callistemon citrinus / Lemon Bottlebrush - Spacing 3' x 6" Dioscorea oppositifolia / Yucca - Spacing 3' x 6" Heterostemma arborescens / Yucca - Spacing 4' x 6" Lonicera caerulea / California Laurel - Spacing 5' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6"	12,734 x 4
	<b>WQMP BASIN PLANTING PALETTE</b> Banksia integrifolia / Australian Banksia / Lemon Biscuit Elmwood - Spacing 3' x 6" Callistemon citrinus / Lemon Bottlebrush - Spacing 3' x 6" Dioscorea oppositifolia / Yucca - Spacing 3' x 6" Heterostemma arborescens / Yucca - Spacing 4' x 6" Lonicera caerulea / California Laurel - Spacing 5' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6"	46,750 x 4
	<b>SHRUB PALETTE - LOW WATER USE</b> Agave americana / Century Plant - Spacing 6' x 6" Atriplex confertifolia / Shrub Salt-Tolerant - Spacing 4' x 6" Daphne genkwa / Grey Daphne - Spacing 5' x 6" Lonicera caerulea / California Laurel - Spacing 5' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6"	31,074 x 4
	<b>GROUND COVER PALETTE - LOW WATER USE - 1 GAL</b> Banksia integrifolia / Australian Banksia / Lemon Biscuit Elmwood - Spacing 3' x 6" Callistemon citrinus / Lemon Bottlebrush - Spacing 3' x 6" Dioscorea oppositifolia / Yucca - Spacing 3' x 6" Heterostemma arborescens / Yucca - Spacing 4' x 6" Lonicera caerulea / California Laurel - Spacing 5' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6"	167,617 x 4
	<b>GROUND COVER PALETTE - MEDIUM WATER USE - 1 GAL</b> Banksia integrifolia / Australian Banksia / Lemon Biscuit Elmwood - Spacing 3' x 6" Callistemon citrinus / Lemon Bottlebrush - Spacing 3' x 6" Dioscorea oppositifolia / Yucca - Spacing 3' x 6" Heterostemma arborescens / Yucca - Spacing 4' x 6" Lonicera caerulea / California Laurel - Spacing 5' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6" Pittosporum tenuifolium / Silver Cholla - Spacing 3' x 6"	14,081 x 4
	<b>GRASS - HIGH WATER USE</b>	2,338 x 4

### REFERENCE NOTES SCHEDULE

SYMBOL	DESCRIPTION	QTY
1	3" THICK X 54" DECORATIVE DRIVEL IMPACTIVE GOLDI WITH RANDOM ACCENT BUILDERS (DE-REX SAND) - OVER WEED BARRIER	18,450 sq
2	6" X 6" CONCRETE MONOLITH TO SEPARATE SHRUB AREAS FROM DRIVEWAY	
3	RIVER ROCK CORBLE # - 12"	35,307 sq

NOTE:  
1. ALL TREES WITHIN 5' OF HARDSCAPE SHALL BE IN A SHADYPART LAYER (WQMP) AREAS NOT ALLOWING ROOT BARRIERS OR HIGH LUMEN ROOT BARRIERS SHALL BE CONTAINED ON THESE AND EXTEND 5' IN BOTH DIRECTIONS FOR A TOTAL OF 10'  
2. CONTRACTOR TO INSTALL CONCRETE MONOLITH BETWEEN SHRUB AREAS AND HARD PAVING



MATCHLINE SEE SHEET L-3

2011  
**Enviro**  
 LANDSCAPE ARCHITECTS  
 11777 LOS ANGELES BLVD. SUITE 100  
 LOS ANGELES, CA 90049  
 TEL: (310) 979-8000

PREPARED BY:  
**PROFICIENCY 215 LLC**  
 11777 LOS ANGELES BLVD. SUITE 100  
 LOS ANGELES, CA 90049  
 TEL: (310) 979-8000

SHEET TITLE: **PRELIMINARY LANDSCAPE**  
 PROJECT: **PROFICIENCY 215 LLC**  
 0000 ALESSANDRO BLVD.  
 FREEMAN BUSINESS CENTER  
 MARCH, IFA, CA

DRAWN BY: **BFX** DATE: **12/04/14**  
 JOB NO.: **RG40055**  
 SHEET NO.: **L-4**  
 4 of 4 SHEETS

## Guerin, John

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**From:** Clayton Corwin <ccorwin@stonecreekcompany.com>  
**Sent:** Monday, February 02, 2015 4:04 PM  
**To:** Guerin, John  
**Subject:** ALUC Agenda for 2/5/15 -- March Air Reserve Base, Item 3.3 -- ZAP1107MA14

Hi John — We just received notice of this meeting, and we were finally able to obtain a copy of the proposed site plan. We own the property immediately adjacent to the south and east of the subject property. We have very limited frontage on Old 215 Frontage Road; the southerly proposed access point for the subject property appears to be at our common property line. Due to typical traffic requirements for driveway separation distances, this design will likely render our property undevelopable. Accordingly, we object to its location and the site plan, and respectfully request a change in this access design to solve this problem.

Thank you,

Clayton M. Corwin  
StoneCreek Company  
30212 Tomas | Suite 300  
Rancho Santa Margarita | CA | 92688  
tel 949.709.8080 | fax 949.709.8081 | cell 949.874.6033

## Guerin, John

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**From:** Grace Williams <williams@marchjpa.com>  
**Sent:** Wednesday, March 04, 2015 9:50 AM  
**To:** Pam Steele; STRICKLAND, WILLIAM K GS-12 USAF AFRC 452 MSG/CEV; Denise Hauser; Sonia Pierce; Gary Gosliga; Dan Fairbanks; Guerin, John  
**Cc:** Jeffrey Trenton; Bob Sullivan  
**Subject:** RE: Freeway Business Center - Proficiency 215 LLC

Hi Pam,

Thank you for this information. To be clear, the information provided in your email illustrates a concept that will be further explored within a hydrology/drainage study, is that correct? I just want to be clear that what you are looking for is MARB's blessing on the proposed concept. Technical studies may result in a different final design, but in line with the proposed concept if the Air Force agrees. Is this a correct assumption?

Also, I am forwarding your email to our WQMP expert to ensure that the proposed BMP is acceptable as per MJPA standards. I will forward her feedback as soon as I hear from her.

Thank you,

### Grace I. Williams

Principal Planner  
March Joint Powers Authority  
23555 Meyer Drive  
Riverside, CA. 92518  
P: (951) 656-7000  
F: (951) 697-6706  
[williams@marchjpa.com](mailto:williams@marchjpa.com)

**From:** Pam Steele [<mailto:pams@migcom.com>]  
**Sent:** Tuesday, March 03, 2015 5:13 PM  
**To:** STRICKLAND, WILLIAM K GS-12 USAF AFRC 452 MSG/CEV; Denise Hauser; Sonia Pierce; Gary Gosliga; Grace Williams; Dan Fairbanks; Guerin, John  
**Cc:** Jeffrey Trenton; Bob Sullivan  
**Subject:** Fwd: Freeway Business Center - Proficiency 215 LLC

All,

Thank you again for meeting with us on February 26 to discuss Air Force concern about the potential for standing water in the project retention basins attracting birds and therefore creating a bird strike hazard. As we discussed in the meeting, we share your concern and in no way want to create such a hazard.

Following our meeting, we have worked with our civil engineers to ensure that the proposed project retention basins will completely drain within any 48 hour period following the completion of a rain event as required by the ALUCP. Any nuisance water which potentially remains after such 48 hour period will be located well

below a substantial rock layer. The details of our project retention basins and the storm drain system are described in the attached description and exhibits.

Further, since the concern was raised about rising groundwater in the Perris North Sub-basin, we have extensively researched the issue to learn if it presents a risk to our project site. With considerable relief, we have learned that our project site is located beyond the boundaries of the Perris North Sub-basin. As shown on the maps (attached), the northernmost boundary of the Perris North Sub-basin, nearest to our project site, is located at Cactus Avenue, more than 200 feet south of our project site property line. That boundary line then veers sharply northeast from Cactus Avenue, well east of our project site.

However, given Air Force concern about this issue and the potential in the future to possibly create standing water in project retention basins, we are proposing to incorporate an additional BMP (Best Management Practice) in our project Water Quality Management Plan (WQMP) to address this issue. Please see proposed BMP attached to this e-mail. Please note that project WQMPs are recorded covenants that run with the land, are subject to State regulation, and require submission of annual reports to the State for compliance.

We are most hopeful these we have addressed the Air Force's concerns in a responsible manner. We would appreciate your review and concurrence that these submittals address the concerns so that we can receive a "Consistent" determination for the Plot Plan at the ALUC Board Meeting next Thursday March 12, 2015.

Thank you very much for your assistance!

Pam

**Pam Steele**

*Principal*

**MIG | Hogle Ireland**  
1500 Iowa Avenue, Suite #110  
Riverside, California 92507  
O: [951 787 9222](tel:9517879222) | C: [951 733 5240](tel:9517335240) | [www.migcom.com](http://www.migcom.com)

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## **Guerin, John**

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**From:** Grace Williams <williams@marchjpa.com>  
**Sent:** Wednesday, February 04, 2015 12:20 PM  
**To:** Guerin, John  
**Subject:** Freeway Business Center Information

Hello John,

I am informed that the County parcel associated with the Freeway Business Center is scheduled for approval by County Board on February 10<sup>th</sup>. With regard to our earlier discussions on tomorrow's meeting, March JPA is in support of a continuance by ALUC if that is what County Counsel and your management recommends. We defer to the Applicant on this matter as it is their Project and they need to work closely with you in your assessment. Thank you for your time.

Best regards,

### **Grace I. Williams**

Senior Planner

#### **March Joint Powers Authority**

23555 Meyer Drive

Riverside, CA. 92518

P: (951) 656-7000

F: (951) 697-6706

[williams@marchjpa.com](mailto:williams@marchjpa.com)

**Guerin, John**

---

3.3

**From:** Grace Williams <williams@marchjpa.com>  
**Sent:** Tuesday, February 03, 2015 1:48 PM  
**To:** Guerin, John  
**Cc:** Pam Steele; David Alvarez; Dan Fairbanks  
**Subject:** Freeway Business Center

Hi John,

I just you a voicemail message regarding the Freeway Business Center. As we discussed this morning, there is a sliver of property along the easterly portion of the Project site that is currently within the County's jurisdiction but is proposed as part of the Project. This month, the Board of Supervisors is considering a the transfer of land use authority on that piece of property over to the March JPA. When that action is completed, the March JPA will then proceed with processing the proposed General Plan Amendment and will be sending over a copy your way for ALUC's consideration. I will let you know what the applicant decides on whether or not to proceed with this Thursday's ALUC meeting for the Project.

When we spoke, you were fine with proceeding with this Thursday's meeting on the proposed Plot Plan and Change of Zone and then scheduling the GPA for a separate Commission meeting at a later time. You were also okay with continuing the case altogether for a later ALUC meeting until the GPA meeting. I've forwarded this information to the applicant's project manager and hope to get direction from them before the end of the day. However, I want to ask you about the Freeway Business Center Change of Zone that you have scheduled for Thursday's meeting. The proposed Project Change of Zone includes the aforementioned parcel sliver within the County's jurisdiction. Will you have any concerns on the proposal?

My thought is, County RCLIS shows that the property within the County jurisdiction is unzoned as with the rest of the Project site. Would it be sufficient for ALUC staff to disclose the jurisdictional divide on the Project but proceed with the finding of consistency? I would think that your findings would be the same regardless of the jurisdictional issue. What are your thoughts?

**Grace I. Williams**  
Senior Planner  
**March Joint Powers Authority**  
23555 Meyer Drive  
Riverside, CA. 92518  
P: (951) 656-7000  
F: (951) 697-6706  
[williams@marchjpa.com](mailto:williams@marchjpa.com)

**Guerin, John**

3.3

**From:** PIERCE, SONIA L CTR USAFR AFRC 452 MSG/CECP <sonia.pierce.ctr@us.af.mil>  
**Sent:** Wednesday, February 04, 2015 2:27 PM  
**To:** Guerin, John; Brady, Russell  
**Cc:** HAUSER, DENISE L GS-11 USAF AFRC 452 MSG/CECP; Grace Williams  
**Subject:** FW: Freeway Business Center

John and Russ,

Denise and I will not be in attendance at the hearing tomorrow due to a MARB presentation that we are required to attend. Denise had discussed the water retention basins with the applicants of ZAP1107MA14. She said the base wanted the same type of covered basins as the MJPA General Terminal has. The standing water has become an attraction for birds and this property is directly under the flight path.

Denise said Gary is familiar with the details of the water detention basin. Before they get too far along, we will contact Gary to get the specs and forward them.

Thank you,

Sonia Pierce  
Community Planner and Liaison  
452 MSG/Civil Engineers  
610 Meyer Drive, Building 2403  
March ARB, CA 92518-2166

Comm: (951) 655-2236

-----Original Message-----

**From:** PIERCE, SONIA L CTR USAFR AFRC 452 MSG/CECP [<mailto:sonia.pierce.ctr@us.af.mil>]  
**Sent:** Monday, December 29, 2014 11:05 AM  
**To:** Grace Williams  
**Cc:** HAUSER, DENISE L GS-11 USAF AFRC 452 MSG/CECP  
**Subject:** Freeway Business Center

Grace,

Did the applicant for the Freeway Business Center (MIG/Hogle-Ireland) provide any details regarding enclosing the water detention basins? Denise recalls they were requested to enclose or cover the water retention basins due to their location in the APZ I and APZ II also the property is right under the flight path (center of the runway).

We are looking for something similar to what MJPA did over at the terminal.

Thanks,

Sonia Pierce

# NOTICE OF PUBLIC HEARING

## RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14<sup>th</sup> Floor, Riverside, California 92501, Monday through Thursday, from 8:00 a.m. to 5:00 p.m., and by prescheduled appointment on Fridays, from 9:00 a.m. to 5:00 p.m.

PLACE OF HEARING: Riverside County Administration Center  
4080 Lemon St., 1<sup>st</sup> Floor Hearing Room  
Riverside, California

DATE OF HEARING: March 12, 2015

TIME OF HEARING: 9:00 A.M.

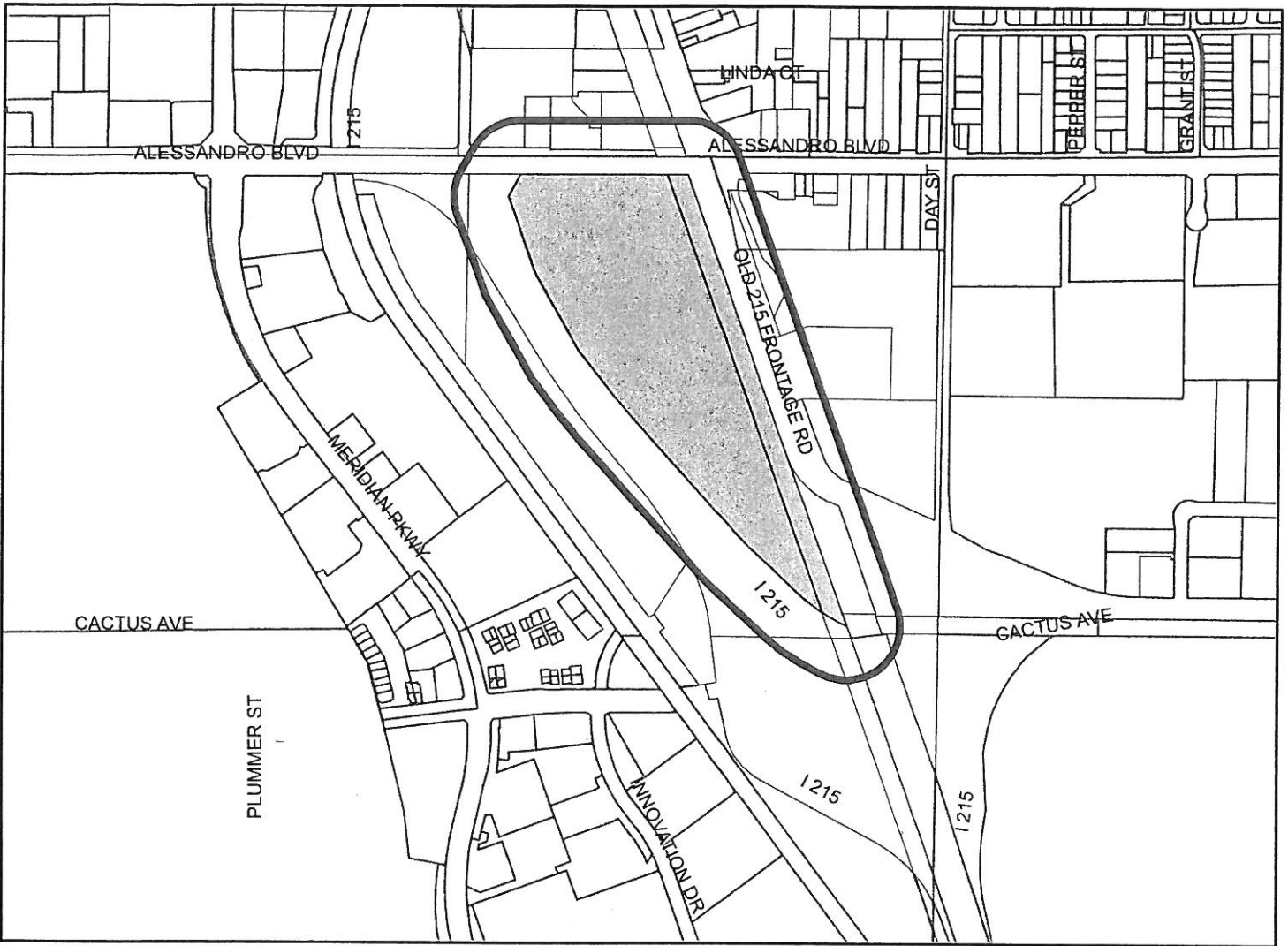
### CASE DESCRIPTION:

ZAP1107MA14 – Proficiency 215 LLC/Proficiency Capital LLC/Jeff Trenton (Representative: Pam Steele, MIG/Hogle-Ireland) – March Joint Powers Authority (JPA) Case Nos. GPA 15-01 (General Plan Amendment), CZ 14-01 (Change of Zone) and PP 14-02 (Plot Plan). A proposal to establish Industrial zoning on 39.42 acres (Assessor's Parcel Nos. 297-100-013 and 297-100-045) located southerly of Alessandro Boulevard, easterly of Interstate 215, westerly of Old 215 Frontage Road, and northerly of Cactus Avenue, and to build a 709,083 square foot industrial warehouse (including 15,000 square feet of office area, 3,000 square feet of which will be on a mezzanine level) thereon. The easterly 6.2 acres (Assessor's Parcel No. 297-100-045) was zoned R-R (Rural Residential) when in County jurisdiction. GPA 15-01 is a proposal to designate the easterly 6.2 acres of the site (Assessor's Parcel No. 297-100-045) as Industrial on the March JPA General Plan. (Airport Compatibility Zones B1-APZ I and B1-APZ II of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan).

FURTHER INFORMATION: Contact John Guerin at (951) 955-0982. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Ms. Grace Williams of the March Joint Powers Authority, at (951) 656-7000.

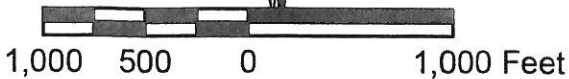


**APN: 297-100-013, 045 (300 feet buffer)**



**Selected Parcels**

263-100-022 263-100-023 297-100-066 297-100-073 297-100-076 297-120-016 297-120-017 297-120-018 263-100-019 297-120-001  
 263-100-014 297-100-057 297-100-059 297-100-006 297-100-013 297-100-045 263-100-021 263-100-020 297-100-079 297-100-005  
 297-100-023 297-100-025 297-110-005 297-110-021 263-100-005 297-100-046 297-110-022 297-100-008 297-100-063



Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

**APPLICATION FOR MAJOR LAND USE ACTION REVIEW**  
**RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**

ALUC Identification No.

ZAP 1107MA14

**PROJECT PROPONENT (TO BE COMPLETED BY APPLICANT)**

Date of Application 12/10/2014  
 Property Owner Proficiency 215 LLC Phone Number (951) 787-9222  
 Mailing Address 11777 San Vicente Blvd.  
Suite 780  
Los Angeles, CA. 90049

Agent (if any) MIG / Hoyle-Ireland (Pam Steele) Phone Number (951) 787-9222  
 Mailing Address 1500 Iowa Avenue  
#110  
Riverside, CA. 92507

**PROJECT LOCATION (TO BE COMPLETED BY APPLICANT)**

*Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways*

Street Address N/A  
 Assessor's Parcel No. 297-100-013, -045 Parcel Size 34.23 Acres  
 Subdivision Name \_\_\_\_\_ Zoning Classification None  
 Lot Number \_\_\_\_\_

**PROJECT DESCRIPTION (TO BE COMPLETED BY APPLICANT)**

*If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed*

Existing Land Use (describe) Vacant Parcel  
 Proposed Land Use (describe) 709,083 square foot Industrial Warehouse

For Residential Uses Number of Parcels or Units on Site (exclude secondary units) None  
 For Other Land Uses Hours of Use Unknown at this time  
 (See Appendix C) Number of People on Site Maximum Number \_\_\_\_\_  
 Method of Calculation \_\_\_\_\_

Height Data Height above Ground or Tallest Object (including antennas and trees) 44'-3" ft.  
 Highest Elevation (above sea level) of Any Object or Terrain on Site \_\_\_\_\_ ft.

Flight Hazards Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?  Yes  No  
 If yes, describe \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_

REFERRING AGENCY (APPLICANT OR JURISDICTION TO COMPLETE)	
Date Received	12/8/14
Agency Name	March Joint Powers Authority
Staff Contact	Grace Williams
Phone Number	(951) 656-7000
Agency's Project No.	CZ 14-01 Plot Plan 14-02
Type of Project	<input type="checkbox"/> General Plan Amendment <input checked="" type="checkbox"/> Zoning Amendment or Variance <input type="checkbox"/> Subdivision Approval <input type="checkbox"/> Use Permit <input type="checkbox"/> Public Facility <input checked="" type="checkbox"/> Other Plot Plan

A. **NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. **SUBMISSION PACKAGE:**

ALUC REVIEW

- 1 . . . . . Completed Application Form
- 1 . . . . . Project Site Plan – Folded (8-1/2 x 14 max.)
- 1 . . . . . Elevations of Buildings - Folded
- 1 Each . 8 ½ x 11 reduced copy of the above
- 1 . . . . . 8 ½ x 11 reduced copy showing project in relationship to airport.
- 1 Set . Floor plans for non-residential projects
- 4 Sets . Gummed address labels of the Owner and representative (**See Proponent**).
- 1 Set . Gummed address labels of all property owners within a 300' radius of the project site. If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10), with ALUC return address.
- 4 Sets . Gummed address labels of the referring agency (City or County).
- 1 . . . . . Check for Fee (See Item "C" below)

STAFF REVIEW (Consult with ALUC staff planner as to whether project qualifies)

- 1 . . . . . Completed Application Form
- 1 . . . . . Project Site Plans – Folded (8-1/2 x 14 max.)
- 1 . . . . . Elevations of Buildings - Folded
- 1 . . . . . 8 ½ x 11 Vicinity Map
- 1 Set . Gummed address labels of the Owner and representative (**See Proponent**).
- 1 Set . Gummed address labels of the referring agency.
- 1 . . . . . Check for review—See Below

## Guerin, John

---

**From:** Pam Steele <pams@migcom.com>  
**Sent:** Wednesday, January 21, 2015 8:29 AM  
**To:** Guerin, John  
**Cc:** Jeffrey Trenton  
**Subject:** Fwd: Status of FAA Filing

John,

Please see the e-mail below identifying that the FAA filing has been initiated.

Thank you,

Pam

### **Pam Steele**

*Principal*

**MIG | Hogle Ireland**  
1500 Iowa Avenue, Suite #110  
Riverside, California 92507  
O: 951 787 9222 | C: 951 733 5240 | [www.migcom.com](http://www.migcom.com)

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----- Forwarded message -----

**From:** J Trenton <[JTrenton@proficiencycapital.com](mailto:JTrenton@proficiencycapital.com)>  
**Date:** Wed, Jan 21, 2015 at 12:29 AM  
**Subject:** Fwd: Status of FAA Filing  
**To:** Pam Steele <[pams@migcom.com](mailto:pams@migcom.com)>, Grace Williams <[williams@marchjpa.com](mailto:williams@marchjpa.com)>  
**Cc:** Mike Gill <[mike@rga-architects.com](mailto:mike@rga-architects.com)>, Bob Sullivan <[rsullivan@thomsenenginc.com](mailto:rsullivan@thomsenenginc.com)>

Hi Grace and Pam -

Please find evidence of the filing of FAA Form 7460-1 for Freeway Business Center.

Regards,

Jeff

Begin forwarded message:

**From:** "noreply@faa.gov" <noreply@faa.gov>  
**Date:** January 20, 2015 at 8:59:19 PM GMT+1  
**To:** J Trenton <JTrenton@proficiencycapital.com>, J Trenton <JTrenton@proficiencycapital.com>  
**Subject:** Status of FAA Filing  
**Reply-To:** "oeaaa\_helpdesk@cghtech.com" <oeaaa\_helpdesk@cghtech.com>

Your filing is assigned Aeronautical Study Number (ASN): 2015-AWP-566-OE.

To review your electronic record, go to our website [oeaaa.faa.gov](http://oeaaa.faa.gov) and select the Search Archives link to locate your case using the assigned Aeronautical Study Number (ASN). Copies of your letter are available on the website for your convenience.

The FAA verified your filing and an aeronautical study has been initiated. Please allow a minimum 45 days for the FAA to complete the study. Please refer to the assigned ASN on all future inquiries regarding this filing.

For Wind Turbine proposals only, please ensure Wind Turbine Data as described on the project summary page in your registered e-filing account has been uploaded to your filing.

*To ensure e-mail notifications are delivered to your inbox please add [noreply@faa.gov](mailto:noreply@faa.gov) to your address book. Notifications sent from this address are system generated FAA e-mails and replies to this address will NOT be read or forwarded for review. Each system generated e-mail will contain specific FAA contact information in the text of the message.*

**Guerin, John**

---

**From:** Pam Steele <pams@migcom.com>  
**Sent:** Wednesday, March 11, 2015 8:17 AM  
**To:** PIERCE, SONIA L CTR USAFR AFRC 452 MSG/CECP  
**Cc:** Guerin, John; Grace Williams; HAUSER, DENISE L GS-11 USAF AFRC 452 MSG/CECP  
**Subject:** Re: Freeway Business Center

Sonia,

Thank you for providing this memo. Please confirm - in reading the memo, it appears that ALUC would be able to make a finding that the Plot Plan is Consistent - with conditions added as outlined in this memo and the additional BMP. Is that correct?

Thanks again,

Pam

**Pam Steele**

*Principal*

**MIG | Hogle Ireland**  
1500 Iowa Avenue, Suite #110  
Riverside, California 92507  
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On Wed, Mar 11, 2015 at 7:59 AM, PIERCE, SONIA L CTR USAFR AFRC 452 MSG/CECP  
<[sonia.pierce.ctr@us.af.mil](mailto:sonia.pierce.ctr@us.af.mil)> wrote:

John,

After talking with staff, this letter was written with the understanding that the monitoring will be included in the BPM.

Respectfully,

Sonia Pierce  
Community Planner and Liaison  
452 MSG/Civil Engineers  
610 Meyer Drive, Building 2403  
March ARB, CA 92518-2166

Comm: [\(951\) 655-2236](tel:(951)655-2236)

## Guerin, John

---

**From:** Pam Steele <pams@migcom.com>  
**Sent:** Thursday, March 05, 2015 11:25 AM  
**To:** STRICKLAND, WILLIAM K GS-12 USAF AFRC 452 MSG/CEV  
**Cc:** Denise Hauser; Sonia Pierce; Gary Gosliga; Dan Fairbanks; Guerin, John; Jeffrey Trenton; Bob Sullivan; Grace Williams  
**Subject:** Re: Freeway Business Center - Proficiency 215 LLC

Will, Denise, Sonia, and Gary,

Hopefully you have had a chance to review the information we sent. Since none of it was in the form of a condition of approval, we would like to propose that a condition be added to project. The condition could be something like:

- In order to ensure proper function of the project drain system to avoid potential hazards to March Air Reserve Base flights, an additional Best Management Practice (BMP) shall be added to the project Water Quality Management Plan (WQMP), which will be recorded with the land. The additional BMP will require that the retention basins be maintained and inspected to prevent water ponding in the basins after 48 hours following a rain event. Should such ponding or standing water occur, the BMP will require an engineered solution to be developed and constructed.

If you have suggestions for revision to this condition - or to the BMP, please let us know. For instance, as currently written, the schedule for the proposed BMP states "Annually" - if you have concerns about this timing and feel that should be assessed 48 hours after every rain event, the BMP could be modified to so state.

Please let us know if you would like to discuss - we can schedule a conference call or another meeting if you would like.

Thanks again,

Pam

**Pam Steele**

*Principal*

**MIG | Hogle Ireland**  
1500 Iowa Avenue, Suite #110  
Riverside, California 92507  
O: 951 787 9222 | C: 951 733 5240 | [www.migcom.com](http://www.migcom.com)

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On Wed, Mar 4, 2015 at 9:54 AM, Pam Steele <[pams@migcom.com](mailto:pams@migcom.com)> wrote:  
Grace,

Yes, your assumption is correct. The hydrology study is almost complete and the design concept is pretty solid at this point.

In order to solidify the WQMP BMP, we would anticipate a condition of approval added to the project - by both the ALUC and March JPA - to add the BMP to the WQMP. That would be pending approval by your WQMP engineer, of course.

Is it possible to get her feedback today, as time is critical to get this resolved in time for the ALUC meeting?

Thank you!

Pam

**Pam Steele**

*Principal*

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On Wed, Mar 4, 2015 at 9:49 AM, Grace Williams <[williams@marchjpa.com](mailto:williams@marchjpa.com)> wrote:

Hi Pam,

Thank you for this information. To be clear, the information provided in your email illustrates a concept that will be further explored within a hydrology/drainage study, is that correct? I just want to be clear that what you are looking for is MARB's blessing on the proposed concept. Technical studies may result in a different final design, but in line with the proposed concept if the Air Force agrees. Is this a correct assumption?

Also, I am forwarding your email to our WQMP expert to ensure that the proposed BMP is acceptable as per MJPA standards. I will forward her feedback as soon as I hear from her.

Thank you,

## Grace I. Williams

Principal Planner

### March Joint Powers Authority

23555 Meyer Drive

Riverside, CA. 92518

P: [\(951\) 656-7000](tel:(951)656-7000)

F: [\(951\) 697-6706](tel:(951)697-6706)

[williams@marchjpa.com](mailto:williams@marchjpa.com)

**From:** Pam Steele [mailto:[pams@migcom.com](mailto:pams@migcom.com)]

**Sent:** Tuesday, March 03, 2015 5:13 PM

**To:** STRICKLAND, WILLIAM K GS-12 USAF AFRC 452 MSG/CEV; Denise Hauser; Sonia Pierce; Gary Gosliga; Grace Williams; Dan Fairbanks; Guerin, John

**Cc:** Jeffrey Trenton; Bob Sullivan

**Subject:** Fwd: Freeway Business Center - Proficiency 215 LLC

All,

Thank you again for meeting with us on February 26 to discuss Air Force concern about the potential for standing water in the project retention basins attracting birds and therefore creating a bird strike hazard. As we discussed in the meeting, we share your concern and in no way want to create such a hazard.

Following our meeting, we have worked with our civil engineers to ensure that the proposed project retention basins will completely drain within any 48 hour period following the completion of a rain event as required by the ALUCP. Any nuisance water which potentially remains after such 48 hour period will be located well below a substantial rock layer. The details of our project retention basins and the storm drain system are described in the attached description and exhibits.

Further, since the concern was raised about rising groundwater in the Perris North Sub-basin, we have extensively researched the issue to learn if it presents a risk to our project site. With considerable relief, we have learned that our project site is located beyond the boundaries of the Perris North Sub-basin. As shown on the maps (attached), the northernmost boundary of the Perris North Sub-basin, nearest to our project site, is located at Cactus Avenue, more than 200 feet south of our project site property line. That boundary line then veers sharply northeast from Cactus Avenue, well east of our project site.

However, given Air Force concern about this issue and the potential in the future to possibly create standing water in project retention basins, we are proposing to incorporate an additional BMP (Best Management Practice) in our project Water Quality Management Plan (WQMP) to address this issue. Please see proposed BMP attached to this e-mail. Please note that project WQMPs are recorded covenants that run with the land, are subject to State regulation, and require submission of annual reports to the State for compliance.

We are most hopeful these we have addressed the Air Force's concerns in a responsible manner. We would appreciate your review and concurrence that these submittals address the concerns so that we can receive a "Consistent" determination for the Plot Plan at the ALUC Board Meeting next Thursday March 12, 2015.

Thank you very much for your assistance!

Pam

**Pam Steele**

*Principal*

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## Guerin, John

---

**From:** Pam Steele <pams@migcom.com>  
**Sent:** Wednesday, March 04, 2015 9:55 AM  
**To:** Grace Williams  
**Cc:** STRICKLAND, WILLIAM K GS-12 USAF AFRC 452 MSG/CEV; Denise Hauser; Sonia Pierce; Gary Gosliga; Dan Fairbanks; Guerin, John; Jeffrey Trenton; Bob Sullivan  
**Subject:** Re: Freeway Business Center - Proficiency 215 LLC

Grace,

Yes, your assumption is correct. The hydrology study is almost complete and the design concept is pretty solid at this point.

In order to solidify the WQMP BMP, we would anticipate a condition of approval added to the project - by both the ALUC and March JPA - to add the BMP to the WQMP. That would be pending approval by your WQMP engineer, of course.

Is it possible to get her feedback today, as time is critical to get this resolved in time for the ALUC meeting?

Thank you!

Pam

### **Pam Steele**

*Principal*

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On Wed, Mar 4, 2015 at 9:49 AM, Grace Williams <[williams@marchjpa.com](mailto:williams@marchjpa.com)> wrote:

Hi Pam,

Thank you for this information. To be clear, the information provided in your email illustrates a concept that will be further explored within a hydrology/drainage study, is that correct? I just want to be clear that what you are looking for is MARB's blessing on the proposed concept. Technical studies may result in a different final design, but in line with the proposed concept if the Air Force agrees. Is this a correct assumption?

Also, I am forwarding your email to our WQMP expert to ensure that the proposed BMP is acceptable as per MJPA standards. I will forward her feedback as soon as I hear from her.

Thank you,

**Grace I. Williams**

Principal Planner

**March Joint Powers Authority**

23555 Meyer Drive

Riverside, CA. 92518

P: [\(951\) 656-7000](tel:(951)656-7000)

F: [\(951\) 697-6706](tel:(951)697-6706)

[williams@marchjpa.com](mailto:williams@marchjpa.com)

**From:** Pam Steele [mailto:[pams@migcom.com](mailto:pams@migcom.com)]

**Sent:** Tuesday, March 03, 2015 5:13 PM

**To:** STRICKLAND, WILLIAM K GS-12 USAF AFRC 452 MSG/CEV; Denise Hauser; Sonia Pierce; Gary Gosliga; Grace Williams; Dan Fairbanks; Guerin, John

**Cc:** Jeffrey Trenton; Bob Sullivan

**Subject:** Fwd: Freeway Business Center - Proficiency 215 LLC

All,

Thank you again for meeting with us on February 26 to discuss Air Force concern about the potential for standing water in the project retention basins attracting birds and therefore creating a bird strike hazard. As we discussed in the meeting, we share your concern and in no way want to create such a hazard.

Following our meeting, we have worked with our civil engineers to ensure that the proposed project retention basins will completely drain within any 48 hour period following the completion of a rain event as required by the ALUCP. Any nuisance water which potentially remains after such 48 hour period will be located well below a substantial rock layer. The details of our project retention basins and the storm drain system are described in the attached description and exhibits.

Further, since the concern was raised about rising groundwater in the Perris North Sub-basin, we have extensively researched the issue to learn if it presents a risk to our project site. With considerable relief, we have learned that our project site is located beyond the boundaries of the Perris North Sub-basin. As shown on the maps (attached), the northernmost boundary of the Perris North Sub-basin, nearest to our project site, is located at Cactus Avenue, more than 200 feet south of our project site property line. That boundary line then veers sharply northeast from Cactus Avenue, well east of our project site.

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We are most hopeful these we have addressed the Air Force's concerns in a responsible manner. We would appreciate your review and concurrence that these submittals address the concerns so that we can receive a "Consistent" determination for the Plot Plan at the ALUC Board Meeting next Thursday March 12, 2015.

Thank you very much for your assistance!

Pam

**Pam Steele**

*Principal*

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## Guerin, John

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**From:** Pam Steele <pams@migcom.com>  
**Sent:** Tuesday, March 03, 2015 5:13 PM  
**To:** STRICKLAND, WILLIAM K GS-12 USAF AFRC 452 MSG/CEV; Denise Hauser; Sonia Pierce; Gosliga@MarchJPA.com; Grace Williams; Dan Fairbanks; Guerin, John  
**Cc:** Jeffrey Trenton; Bob Sullivan  
**Subject:** Fwd: Freeway Business Center - Proficiency 215 LLC  
**Attachments:** Freeway Business Center storm drain system description and exhibits.pdf; north perris sub basin.pdf; Additional BMP for WQMP - 3-3-15.docx

All,

Thank you again for meeting with us on February 26 to discuss Air Force concern about the potential for standing water in the project retention basins attracting birds and therefore creating a bird strike hazard. As we discussed in the meeting, we share your concern and in no way want to create such a hazard.

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concerns so that we can receive a "Consistent" determination for the Plot Plan at the ALUC Board Meeting next Thursday March 12, 2015.

Thank you very much for your assistance!

Pam

**Pam Steele**

*Principal*

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Freeway Business Center

March JPA

Additional BMP to be added to WQMP:

Schedule	Inspection and Maintenance Activity
<b>Annually</b> , but not during a rain event or the 48 hour period following completion of such rain event.	<ul style="list-style-type: none"><li>• Inspect detention basin(s) for evidence of any ponding water that might attract birds.</li><li>• If, in spite of regular maintenance, such inspection determines that a detention basin no longer drains as originally designed to prevent ponding water that might attract birds, then owner shall promptly engage a licensed civil engineer to prepare a design plan to eliminate ponding water that might attract birds (other than during a rain event and the 48 hour period following the completion of such rain event). Following completion of such design plan, owner shall promptly cause such design to be constructed to such detention basin.</li></ul>

**Guerin, John**

---

**From:** Pam Steele <pams@migcom.com>  
**Sent:** Wednesday, December 17, 2014 4:51 PM  
**To:** Santos, Barbara  
**Cc:** Guerin, John; Brady, Russell  
**Subject:** RE: March JPA - Freeway Business Center plans for ALUC application 1 of 2

Excellent! Thank you Barbara.

On Dec 17, 2014 4:49 PM, "Santos, Barbara" <[BASANTOS@rctlma.org](mailto:BASANTOS@rctlma.org)> wrote:

Hello Pam,

Received and ran copies of 1 and 2 site plans.

Thanks,

Barbara - ALUC

**From:** Pam Steele [mailto:[pams@migcom.com](mailto:pams@migcom.com)]  
**Sent:** Wednesday, December 17, 2014 4:20 PM  
**To:** Santos, Barbara  
**Subject:** Re: March JPA - Freeway Business Center plans for ALUC application 1 of 2

Barbara,

Here is 2 of 2. Please confirm receipt - the Grading plans are pretty hefty as well.

Thanks,

Pam

**Pam Steele**

*Principal*

**MIG | Hogle Ireland**  
1500 Iowa Avenue, Suite #110  
Riverside, California 92507  
O: [951 787 9222](tel:9517879222) | C: [951 733 5240](tel:9517335240) | [www.migcom.com](http://www.migcom.com)

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On Wed, Dec 17, 2014 at 4:11 PM, Santos, Barbara <[BASANTOS@rctlma.org](mailto:BASANTOS@rctlma.org)> wrote:

Okay, thanks!

**From:** Pam Steele [<mailto:pams@migcom.com>]  
**Sent:** Wednesday, December 17, 2014 4:10 PM  
**To:** Santos, Barbara  
**Subject:** March JPA - Freeway Business Center plans for ALUC application 1 of 2

Barbara,

Attached are the plans for 8-1/2 x 11 printing for the application. In case the size of the attachments is too large, I will send in two separate e-mails.

Thank you for printing these for the application. Please let me know if you need anything else.

Pam

**Pam Steele**

*Principal*

**MIG | Hogle Ireland**  
1500 Iowa Avenue, Suite #110  
Riverside, California 92507  
O: [951 787 9222](tel:9517879222) | C: [951 733 5240](tel:9517335240) | [www.migcom.com](http://www.migcom.com)

**COUNTY OF RIVERSIDE  
AIRPORT LAND USE COMMISSION**

**STAFF REPORT**

**AGENDA ITEM:** 3.1

**HEARING DATE:** April 9, 2015

**CASE NUMBER:** ZAP1111MA15 – HHI Riverside, LLC (Representative: Joe Mineo)

**APPROVING JURISDICTION:** County of Riverside

**JURISDICTION CASE NO:** PP25699 (Plot Plan), VAR01893 (Variance)

**MAJOR ISSUES:** None

**RECOMMENDATION:** Staff recommends a finding of CONDITIONAL CONSISTENCY for the Plot Plan and Variance, subject to the conditions included herein and such additional conditions as may be required by the Federal Aviation Administration (FAA) Obstruction Evaluation Service.

**PROJECT DESCRIPTION:** Plot Plan No. 25699 would authorize development of two retail commercial buildings with a total floor area of 12,872 square feet (one of which would include a 1,440 square foot coffee shop served by a drive-thru lane) and two freestanding restaurants with drive-thru (3,434 and 3,252 square feet, respectively, one of which would be a Farmer Boys). Variance Case No. 1893 is a proposal to exceed the allowable number, height, and surface area of on-site advertising signs. Specifically, the applicant proposes a pylon sign 75 feet in height with a total display area of 540 square feet, in addition to two freestanding monument signs.

**PROJECT LOCATION:** The site is located northerly of Cajalco Expressway, easterly of Harvill Avenue, southerly of Messenia Lane, and westerly of Interstate-215, within the unincorporated community of Mead Valley, approximately 8,250 feet southerly of the southerly end of Runway 14-32 at March Air Reserve Base.

**LAND USE PLAN:** 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

- a. Airport Influence Area: March Air Reserve Base
- b. Land Use Policy: Zone C2
- c. Noise Levels: Below 60 CNEL from aircraft

**BACKGROUND:**

Non-Residential Average Land Use Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zone C2. Zone C2 limits average intensity to 200 people per acre. There are no risk-reduction design bonuses available, as March Air Reserve Base/Inland Port Airport is primarily utilized by large aircraft weighing more than 12,500 pounds.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, and March Air Reserve Base/Inland Port Airport Compatibility Plan Policy 2.4, the following rates were used to calculate the occupancy for each proposed building:

- Dining/serving area – 1 person per 15 square feet
- Commercial kitchen – 1 person per 200 square feet
- Retail – 1 person per 115 square feet

Based on the site plan provided, Building A includes 1,139 square feet of dining area and 2,113 square feet of kitchen area for an estimated occupancy of 87 people. Building B includes 1,202 square feet of dining area and 2,113 square feet of kitchen area for an estimated occupancy of 91 people. Building C includes 720 square feet of coffee shop serving area, 720 square feet of kitchen, and 2,540 square feet of retail area for an estimated occupancy of 74 people. Building D includes 8,892 square feet of retail for an estimated occupancy of 77 people. In total there is an estimated occupancy of 329 people. Based on the 5.06 gross acres, this results in an average intensity of 65 people per acre, which is compatible with the Zone C2 average acre criterion of 200.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per standard vehicle in the absence of more precise data). Based on the number of parking spaces provided of 158, the total occupancy would be estimated at 237 people. Based on the 5.06 gross acres, this results in an average intensity of 47 people per acre, which is also compatible with the Zone C2 average acre criterion of 200.

Non-Residential Single-Acre Land Use Intensity: Compatibility Zone C2 limits maximum single-acre intensity to 500 people. There are no risk-reduction design bonuses available, as March Air Reserve Base/Inland Port Airport is primarily utilized by large aircraft weighing more than 12,500 pounds.

Based on the site plan provided and the occupancies as previously noted, the total occupancy would not exceed the single-acre criterion of 500 people. Even if all of the buildings were located within a single-acre, the estimated occupancy of 329 people would still not exceed the single-acre criterion. Staff estimates the highest occupancy single-acre area (including Buildings C and D) would result in an occupancy of 151 people.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zone C2.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being in an area below the 60 CNEL range from aircraft noise. Therefore, the proposed development would not require special measures to mitigate aircraft-generated noise.

Part 77: The elevation of Runway 14-32 at its southerly terminus is approximately 1488 feet above mean sea level (1488 feet AMSL). At a distance of approximately 8,250 feet from the runway, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1570.5 feet AMSL. The project proposes a maximum finished floor elevation of 1522.6 feet AMSL. The proposed buildings have a maximum height of 28.5 feet for a potential maximum building elevation of 1551.1 feet AMSL. However, the project also proposes a 75 foot tall pylon sign with a base height of approximately 1519.6 feet AMSL for a potential maximum structure elevation of 1594.6 feet AMSL. Therefore, review by the FAA Obstruction Evaluation Service is required for the pylon sign. The applicant submitted Form 7460-1 for the permanent 75 foot pylon sign and it has been assigned an Aeronautical Study Number (ASN) of 2015-AWP-2773-OE. At the time of writing of this staff report, no determination has been issued by FAA.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

#### **CONDITIONS:**

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash

transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)

- (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Noise sensitive outdoor nonresidential uses and hazards to flight.
3. The attached notice shall be given to all prospective purchasers and/or tenants of the property and shall be recorded as a deed notice.
  4. Any proposed detention basin(s) on the site (including bioretention areas for water quality treatment) shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the retention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the retention basin(s) shall not include trees that produce seeds, fruits, or berries.

The bioretention area(s) shall be inspected after each significant rainfall event. In the event that standing water is observed in the bioretention area(s) beyond the 48-hour detention period, upon notification to either the United States Air Force or the March Inland Port Airport Authority (the “airport operators”), the airport operators shall notify HHI Riverside, LLC (or its successor(s)-in-interest) (hereafter referred to as “Owner”) in writing

The Owner shall be required to take all measures necessary as soon as possible, but not later than 15 days after written notice, to either drain or cover the standing water. Should the Owner fail to take such action, Owner authorizes the airport operators to take such action as may be necessary to eliminate a hazard to flight. The Owner shall work with the airport operators to prevent recurrence of standing water situations beyond the 48-hour detention period. For each such incidence made known to the Owner, the necessary remediation shall only be considered to have been fulfilled when the airport operators state in writing that the situation has been remediated to the airport operators’ satisfaction.

5. Prior to issuance of a building permit for the proposed 75-foot tall pylon sign, the applicant shall have received a “Determination of No Hazard to Air Navigation” from the Federal Aviation Administration Obstruction Evaluation Service.
6. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave

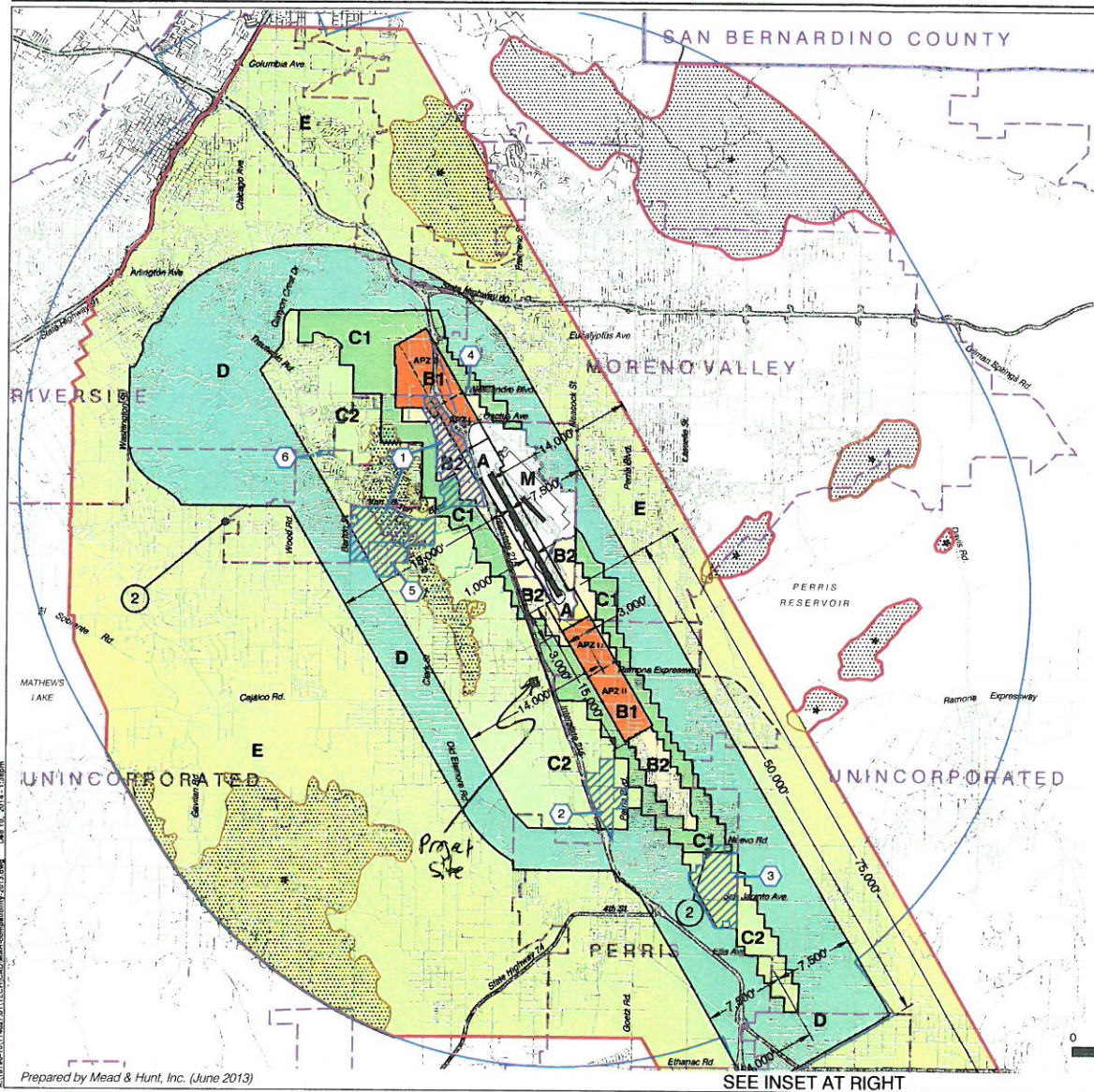
transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.

Y:\AIRPORT CASE FILES\March\ZAP1111MA15\ZAP1111MA15sr.doc



# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



**LEGEND**

**Compatibility Zones**

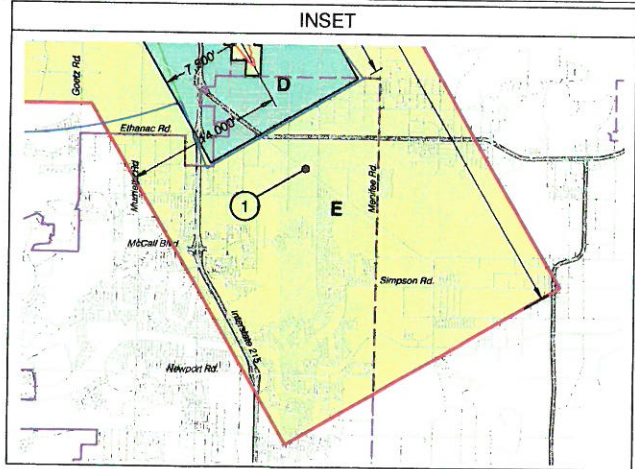
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

**Boundary Lines**

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision

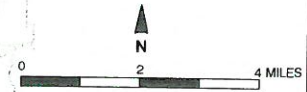


**Riverside County  
Airport Land Use Commission  
March Air Reserve Base / Inland Port Airport  
Land Use Compatibility Plan  
(Adopted November 13, 2014)**

Map MA-1

**Compatibility Map  
March Air Reserve Base / Inland Port Airport**

**Note:**  
All dimensions are measured from runway ends and centerlines.



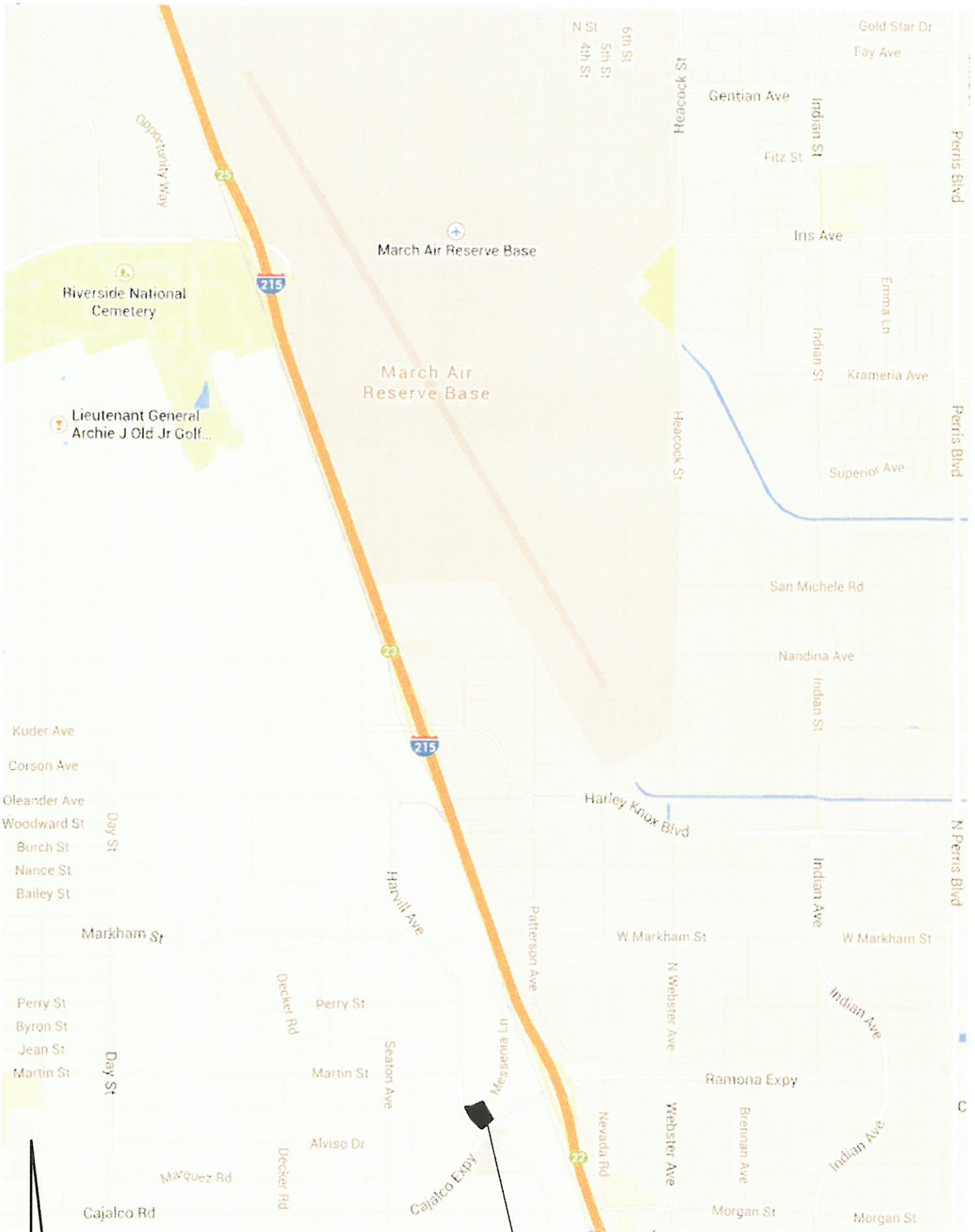
Base map source: County of Riverside 2013

SEE INSET AT RIGHT

X:\BIBS\151116872\GIS\TECH\COMMAP\COMPATIBILITY\_2013.dwg Date: 10/20/14 1:38pm

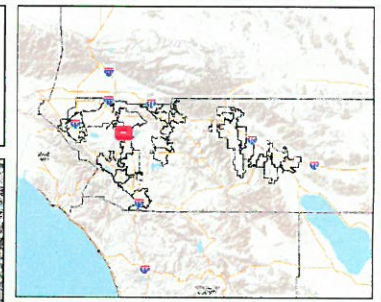
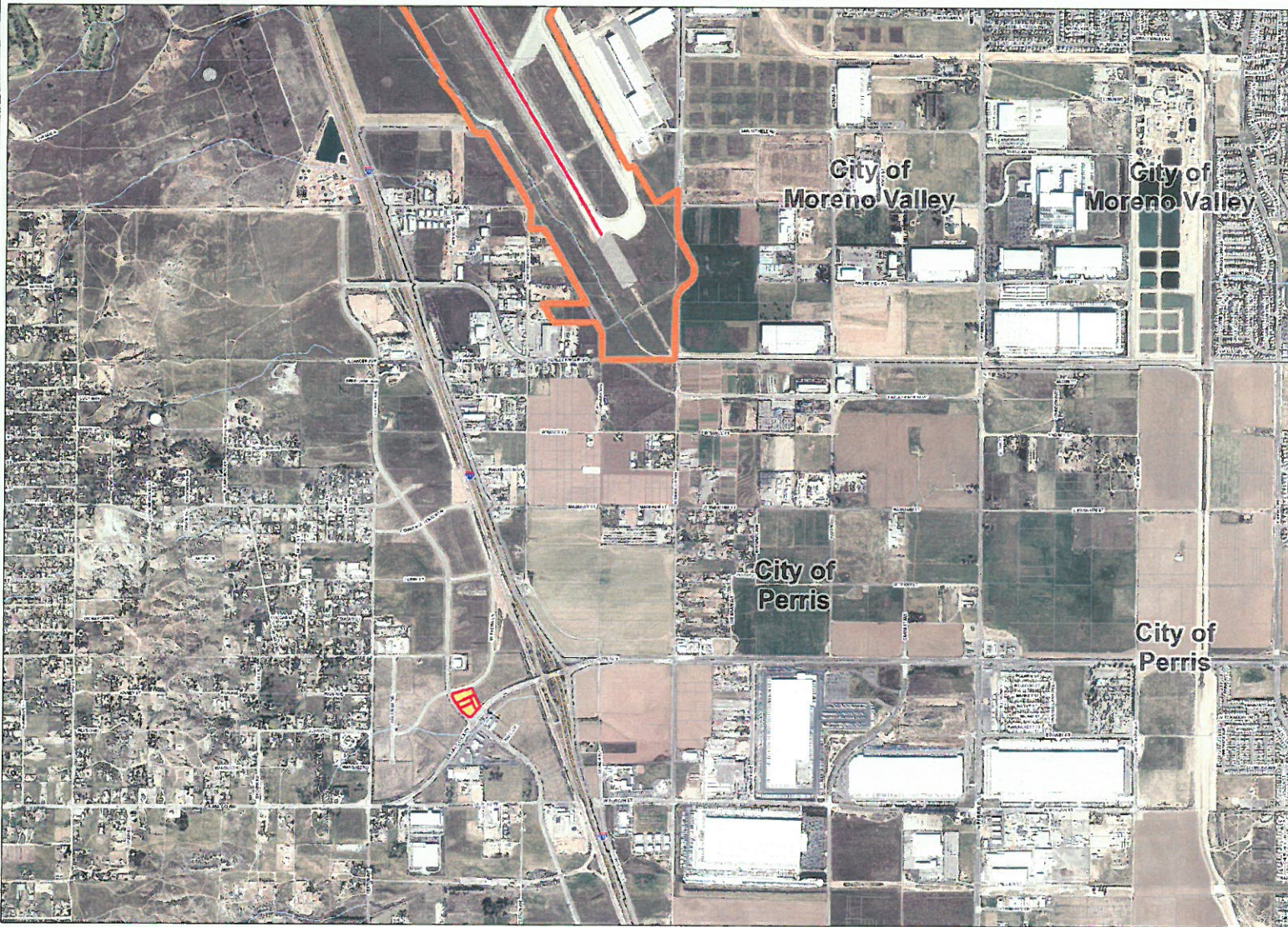
Prepared by Mead & Hunt, Inc. (June 2013)

# PROJECT LOCATION EXHIBIT



**PROJECT SITE**  
A.P.N. 317-110-057, 058, 070, 071

# My Map



## Legend

- Airports
- AIA
- Runways
- City Boundaries
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads**
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities
- hydrographylines**
- waterbodies**
- Lakes
- Rivers



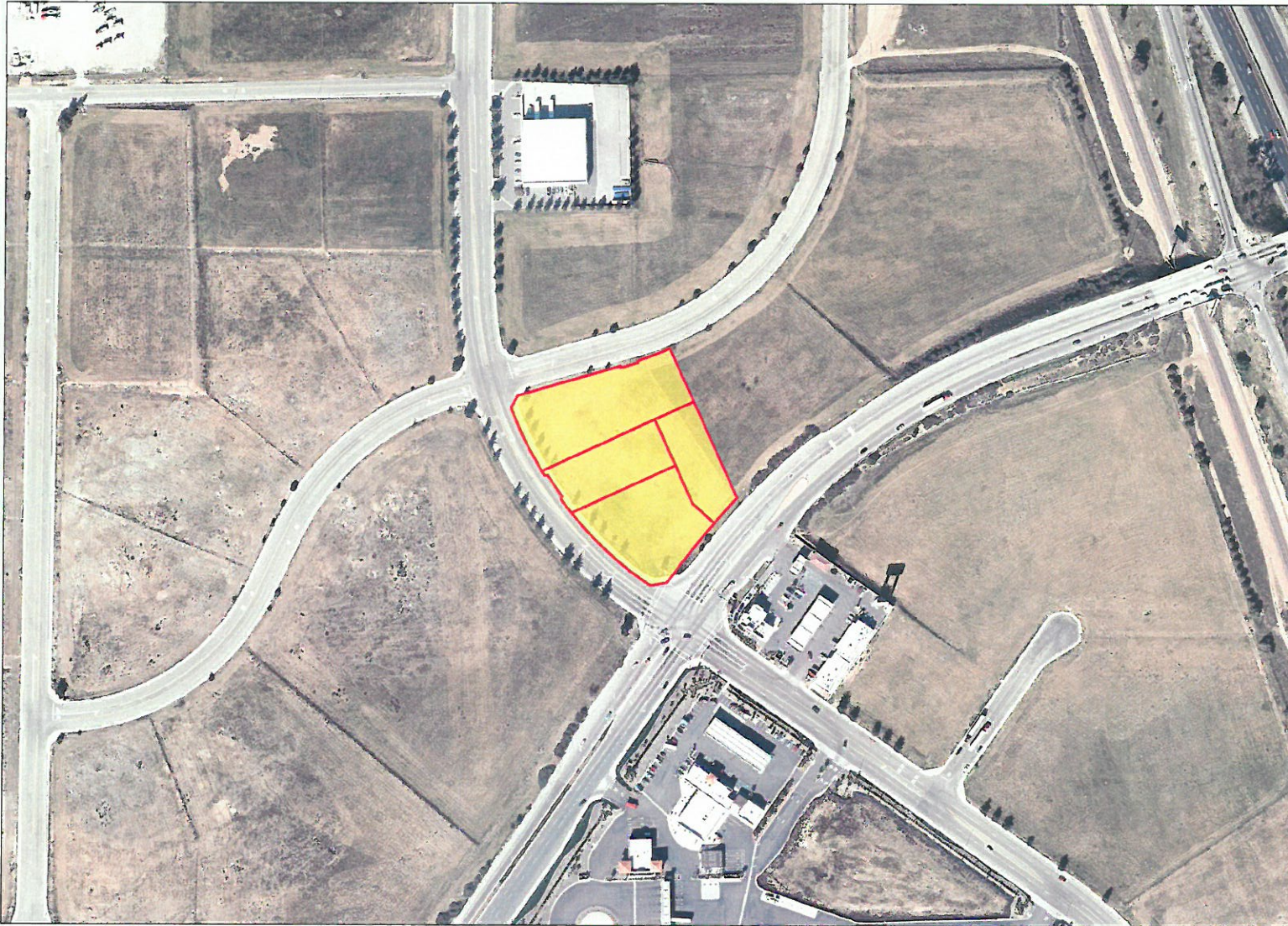
**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 2/24/2015 11:15:48 AM

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## Notes

# My Map



## Legend

- RCLIS Parcels
- Airports
- AIA
- Runways
- City Boundaries
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 411 822 Feet



**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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## Notes

# EXHIBIT 'A'- PLOT PLAN

COUNTY OF RIVERSIDE  
APN 317-110-057, 058, 070 & 071

### Project Description

The project proposes to develop 5.26 gross acres (3.92 net acres) with two three-story restaurants (1,352 SF and 3,454 SF respectively) and two (2) retail/restaurant buildings totaling 11,872 SF that includes a 1,942 SF area for a drive-thru coffee shop. The total development proposal includes 1,912 SF of building area, 912 SF of landscape area, and 200 parking spaces for car and motorcycle treatment. 158 parking spaces, two (2) monument signs and one (1) pylon sign.

A Variance application has been concurrently filed with this application to exceed the height, setback area, and number of signs permitted pursuant to Section 18.4 of Ordinance 948 for an outdoor free-standing advertising sign. Ordinance 948 allows for free-standing outdoor advertising signs to have a maximum height of 20 feet, a minimum surface of 60 feet and no more than two (2) free-standing signs for shopping centers that front on two (2) streets.

The proposed pylon sign is 75 feet in height and has a total display area of 940 SF. This represents a 27-foot high increase in height and 490 square foot increase in display area. The development proposal would include one (1) additional free-standing monument sign for a total of two (2) monument signs and one (1) pylon sign.

### SURROUNDING TOPOGRAPHY (UGS DISPLAY)

**NOTES:**

- ASSESSOR'S PARCEL NUMBER: 317-110-057, 058, 070 & 071
- EXISTING IMPROVED AREA: 200,001 SF = 4.56 AC
- EXISTING PROPOSED ZONING: R-F-3 - SINGLE-FAMILY COMMERCIAL EXISTING LAND USE: VACANT
- PROPOSED LAND USE: COMMERCIAL RETAIL CENTER, INCLUDING THREE EATING ESTABLISHMENTS
- IT IS NOT KNOWN TO BE WITHIN ANY COMMUNITY SERVICE DISTRICT
- SET IS NOT WITHIN A SPECIFIC PLAN
- AREA SUMMARY:
  - TOTAL PROPOSED NET AREA = 148,829 SF = 100%
  - PORION A: PORTION OF APN 317-110-057, 1 PORTION OF 317-110-071 NET = 45,884 SF
  - PORION B: PORTION OF APN 317-110-057, 317-110-058, 1 PORTION OF 317-110-071 NET = 27,550 SF
  - PORION C: PORTION OF APN 317-110-071, 1 PORTION OF 317-110-070 NET = 29,107 SF
  - PORION D: PORTION OF APN 317-110-071 NET = 46,788 SF
- ACCESSORING: 79,309 SF = 53.42% OF TOTAL NET AREA
- LANDSCAPING: 90,764 SF = 61.44% OF TOTAL NET AREA
- BUILDING: 19,358 SF = 13.14% OF TOTAL NET AREA
- TOTAL PARKING AREA: 102,138 SF
- TOTAL PARKING LOT LANDSCAPING: 29,562 SF = 20.05% = 10%
- PARKING SUMMARY:
  - PARCEL A = 3,322 SF
    - DINING AREA = 3,252 SF (10.35) = 1,139 SF @ 1 SPACE / 45 SF = 26 SPACES
    - EMPLOYEES = 4 @ 19 SPACE / 2 EMPLOYEES = 8 SPACES
    - TOTAL PROVIDED = 29
    - TOTAL REQUIRED = 29
    - ACCESSIBLE SPACES PROVIDED: 2
    - ACCESSIBLE SPACES REQUIRED: 2
  - PARCEL B = 3,884 SF
    - DINING AREA = 3,434 SF (10.35) = 1,102 SF @ 1 SPACE / 45 SF = 27 SPACES
    - EMPLOYEES = 6 @ 19 SPACE / 2 EMPLOYEES = 3 SPACES
    - TOTAL PROVIDED = 30
    - TOTAL REQUIRED = 30
    - ACCESSIBLE SPACES PROVIDED: 2
    - ACCESSIBLE SPACES REQUIRED: 2
  - PARCEL C = 3,880 SF
    - DINING AREA = 1,440 SF (10.35) = 220 SF @ 1 SPACE / 45 SF = 16 SPACES
    - EMPLOYEES = 6 @ 19 SPACE / 2 EMPLOYEES = 3 SPACES
    - RETAIL = 1,267 SF @ 1 SPACE / 200 SF = 7 SPACES
    - TOTAL PROVIDED = 33
    - TOTAL REQUIRED = 33
    - ACCESSIBLE SPACES PROVIDED: 2
    - ACCESSIBLE SPACES REQUIRED: 2
  - PARCEL D = 8,891 SF
    - RETAIL = 8,828 SF @ 1 SPACE / 200 SF = 45 SPACES
    - TOTAL PROVIDED = 45
    - TOTAL REQUIRED = 45
    - ACCESSIBLE SPACES PROVIDED: 2
    - ACCESSIBLE SPACES REQUIRED: 2
  - TOTAL REQUIRED PARKING: 113 SPACES
  - TOTAL PROVIDED PARKING: 105 SPACES
  - TOTAL REQUIRED ACCESSIBLE SPACES: 8 SPACES, 4 VAN ACCESSIBLE
  - TOTAL PROVIDED ACCESSIBLE SPACES: 8 SPACES, 4 VAN ACCESSIBLE
11. SITE IS WITHIN FLOOD INSURANCE RATE MAP ZONE X. AREAS DESIGNATED TO BE DEVOIDS OF 2% ANNUAL CHANCE FLOODPLAIN PER MAP PANEL 06056E, 4100, DATED 04/28/2008.
12. SITE IS A LOW TURTLE AND SUBMERGED.
13. SITE IS NOT SUBJECT TO OVERFLOW, INUNDATION OR FLOOD HAZARD.
14. SITE IS SUSCEPTIBLE TO GEOTECHNICAL SUBSIDENCE.

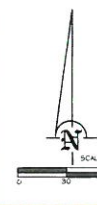
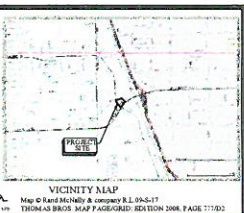
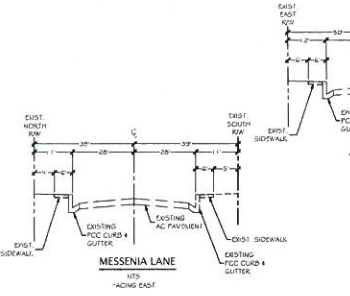
- LEGEND**
- PROPOSED PCC PAVING
  - PROPOSED LANDSCAPE
  - PROPOSED AC PAVING
  - EXISTING PCC PAVING

### LEGAL DESCRIPTION

ALL THAT CERTAIN REAL PROPERTY IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, DESCRIBED AS FOLLOWS: PARCELS 1, 2, 3 AND 4 OF PARCEL MAP NO. 29716, IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS SHOWN ON A MAP FILED IN BOOK 2110, PAGES 12 AND 13 OF PUBLIC MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY, CRYPTING THEREFROM THE ENTIRE MINERAL INTEREST NOT LESS THAN 500.00 FEET BENEATH THE SURFACE, WITHOUT THE RIGHT OF SURFACE ENTRY AS RESERVED BY SANTA FE LAND IMPROVEMENT COMPANY, A CALIFORNIA CORPORATION, IN THE DOCUMENT RECORDED AUGUST 26, 1988 AS INSTRUMENT NO. 244971, OFFICIAL RECORDS.

### EASEMENTS

- AN EASEMENT AS OFFERED FOR DEDICATION IN FAVOR OF PUBLIC UTILITIES. SAID EASEMENT HAS BEEN AMENDED BY INSTRUMENT NO. 2008-2559903, RECORDED OCTOBER 17, 2008.
- THE EFFECT OF A NOTICE OF LOT LINE ADJUSTMENT NO. 048984, RECORDED FEBRUARY 27, 2006 AS INSTRUMENT NO. 2006-0140214, OFFICIAL RECORDS. AMONG OTHER THINGS, SAID DOCUMENT PROVIDED: AN INGRESS AND EGRESS AND PUBLIC UTILITY EASEMENT.
- THE EFFECT OF A DOCUMENT ENTITLED AGREEMENT FOR MAINTENANCE OF PARKWAY FOR INDIVIDUAL, COMMERCIAL AND RETAIL PURPOSES, AFFECTING THOSE PORTIONS OF SAID LAND, AS MORE PARTICULARLY DESCRIBED THEREIN, RECORDED FEBRUARY 26, 2009 AS INSTRUMENT NO. 2009-060004, OFFICIAL RECORDS.
- THE TERMS, CONDITIONS AND PROVISIONS CONTAINED IN THE DOCUMENT ENTITLED "EASEMENT EXECUTED BY J.L.S. ENTERPRISES, LP, A CALIFORNIA LIMITED PARTNERSHIP GRANTS TO THE COUNTY OF RIVERSIDE, A POLITICAL SUBDIVISION, AN EASEMENT FOR PUBLIC ROAD, DRAINAGE AND UTILITY PURPOSES, INCLUDING PUBLIC SERVICES PURPOSES, AFFECTING THOSE PORTIONS OF SAID LAND, AS MORE PARTICULARLY DESCRIBED THEREIN, RECORDED NOVEMBER 19, 2009 AS INSTRUMENT NO. 2009-060004, OFFICIAL RECORDS.
- THE TERMS, CONDITIONS AND PROVISIONS CONTAINED IN THE DOCUMENT ENTITLED "EASEMENT EXECUTED BY J.L.S. ENTERPRISES, LP, A CALIFORNIA LIMITED PARTNERSHIP GRANTS TO THE COUNTY OF RIVERSIDE, A POLITICAL SUBDIVISION, AN EASEMENT FOR PUBLIC ROAD, DRAINAGE AND UTILITY PURPOSES, INCLUDING PUBLIC SERVICES PURPOSES, AFFECTING THOSE PORTIONS OF SAID LAND, AS MORE PARTICULARLY DESCRIBED THEREIN, RECORDED DECEMBER 2, 2009 AS INSTRUMENT NO. 2009-0602941, OFFICIAL RECORDS.
- THE TERMS, CONDITIONS AND PROVISIONS CONTAINED IN THE DOCUMENT ENTITLED "EASEMENT EXECUTED BY J.L.S. ENTERPRISES, LP, A CALIFORNIA LIMITED PARTNERSHIP GRANTS TO THE COUNTY OF RIVERSIDE, A POLITICAL SUBDIVISION, AN EASEMENT FOR PUBLIC ROAD, DRAINAGE AND UTILITY PURPOSES, INCLUDING PUBLIC SERVICES PURPOSES, AFFECTING THOSE PORTIONS OF SAID LAND, AS MORE PARTICULARLY DESCRIBED THEREIN, RECORDED DECEMBER 2, 2009 AS INSTRUMENT NO. 2009-0602941, OFFICIAL RECORDS.



**SOURCE OF SURVEY**  
TOPOGRAFIC SURVEY  
DATED MAY, 2014  
AS CONDUCTED BY:  
ON POINT LAND SURVEYING INC.  
3710 ALABAMA STREET, SUITE A  
REVERSH, CALIFORNIA 92517  
PHONE: (951) 792-2221  
FAX: (951) 754-7246

- LEGEND**
- EXISTING
  - PROPOSED
  - P.T. FIRE INSURANT
  - C.H. CHYR LINDG
  - P.L. PROPERTY LINE
  - R.O. RIGHT OF WAY
  - S.D.M.P. STREET DRAIN MANHOLE
  - S.L. SINKER MANHOLE
  - S.I. STREET LIGHT
  - T.S. TRAFFIC SIGNAL PULBURN
  - E.S. EXISTING STRUCTURE
  - ZONING LEGEND
  - R.F.C. RESIDENTIAL COMMERCIAL ZONE
  - M.S.C. MANUFACTURING - SERVICE COMMERCIAL ZONE

NO.	REVISIONS	DATE	BENCHMARK
1			3" X 4" ALUMINUM DISC SET FLUSH IN THE FOOTING OF A STEEL PIER. FOG LIGHTS "M" MARK (3" X 3" MILE) MARKING CHANGED TO 32" IN THE 2 1/2" FROM TO STATION AVE. AT THE NORTHWEST CORNER OF CAJALCO RD AND STATION AVE. 60 FEET NORTH OF THE PAVED AND 32" FEET EAST OF STATION AVE. ON THE FOOTING OF A LARGE STEEL POWER POLE 7058424 ON UNDER PAVEMENT 0.8 FEET BELOW UPPER FOOTING. ELEVATION = 1557.17
2			BASIS OF BEARING THE CENTERLINE OF MESSENIA LANE FOR FMB D (1) & (2) IS 85.0 DEG N 74°52'52" W

**UTILITY PURVEYORS:**

**GAS:**  
SOUTHERN CALIFORNIA GAS COMPANY  
25420 JEFFERSON AVENUE  
MIRAMONTE, CA 92526  
(800) 427-2200

**ELECTRICITY:**  
SOUTHERN CALIFORNIA Edison  
26100 MERRILL ROAD  
RIVERSIDE, CA 92505  
(951) 928-8245

**SEWER & WATER:**  
DASTON MUNICIPAL WATER DISTRICT  
2270 TRAILHEAD ROAD  
PERRIS, CA 92570  
(909) 928-3777

**TELEPHONE:**  
VERIZON  
150 SOUTH JAVIERA ROAD  
POMONA, CA 92543  
(951) 862-3494

**CABLE:**  
TIME WARNER CABLE  
33071 CEDRON STREET  
LAKE ELDORADO, CA 92532  
(951) 211-1544

**SCHOOL DISTRICT:**  
VAL VERDE UNIFIED SCHOOL DISTRICT  
975 WEST MORGAN STREET  
PERRIS, CA 92571  
PHONE: (951) 945-6100  
FAX: (951) 946-6211

**PROPERTY OWNER:**  
**J.L.S. ENTERPRISES, LP, A CALIFORNIA LIMITED PARTNERSHIP**  
601 W. 400th AVE  
3800 DRAPER STREET, SUITE 250  
REVERSH, CA 92501  
PHONE: (951) 752-0200

**PREPARED FOR/APPLICANT:**  
**HHI RIVERSIDE, LLC**  
ATTN: JOE WAMES  
3800 DRAPER STREET, SUITE 250  
REVERSH, CA 92501  
PHONE: (951) 752-0255

**EXHIBIT 'A' - PLOT PLAN**  
COUNTY OF RIVERSIDE  
APN 317-110-057, 058, 070 & 071

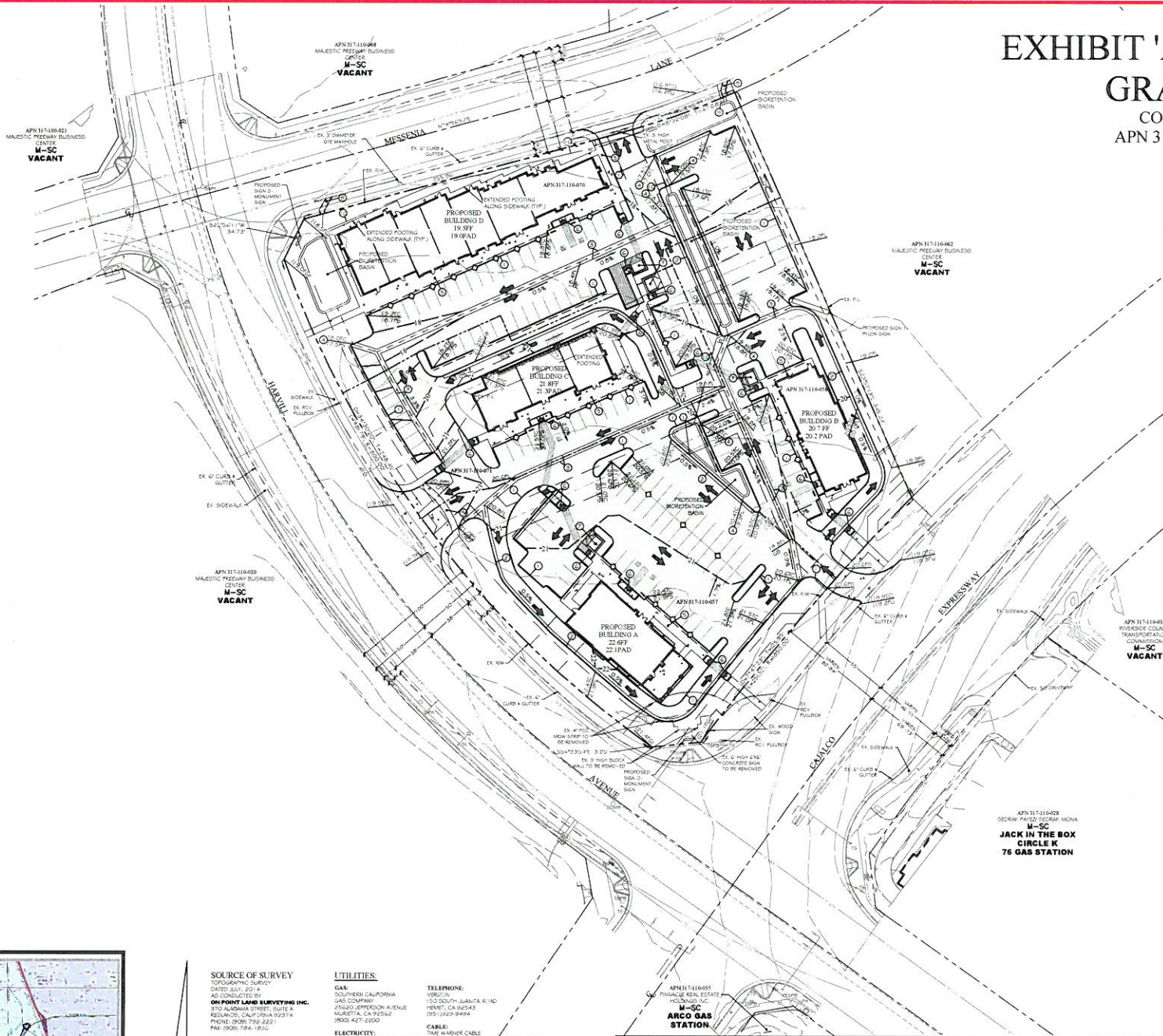
• land planning  
• civil engineering  
• landscape architecture  
• site architecture  
• construction management  
• interior design

**thatcher engineering & associates, inc.**  
1410 S. GARDEN ST., SUITE 100  
RIVERSIDE, CA 92507  
PHONE: (951) 514-1111  
FAX: (951) 514-1112

300 Number: 113-1056 Date Prepared: 11/16/14 Drawn By: JLK Revision Number: 13-10650P

# EXHIBIT 'A.1'- CONCEPTUAL GRADING PLAN

COUNTY OF RIVERSIDE  
 APN 317-110-057, 058, 070 & 071



- KEY NOTES:**
- PROPOSED CURB
  - PROPOSED CURB & GUTTER
  - PROPOSED RIBBON GUTTER
  - PROPOSED CURB OPENING
  - PROPOSED PCC SIDEWALK
  - PROPOSED ADA BUMP
  - PROPOSED TRASH ENCLOSURE
  - PROPOSED UNDER SIDEWALK DRAIN
  - PROPOSED SLUMP & PUMP
  - PROPOSED STORM DRAIN

- LEGEND**
- ▭ PROPOSED PCC PAVING
  - ▭ PROPOSED AC PAVING
  - ▭ EXISTING PCC PAVING

**LEGAL DESCRIPTION**

ALL THAT CERTAIN REAL PROPERTY IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, DESCRIBED AS FOLLOWS:  
 PARCELS 1, 2, 3 AND 4 OF PARCEL MAP NO. 297146, IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS SHOWN ON A MAP FILED IN BOOK 210, PAGES 12 AND 13 OF PARCELS MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

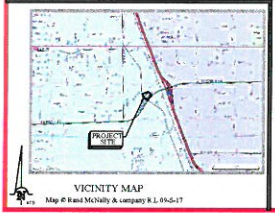
EXCEPTING THEREFROM THE ENTIRE MINERAL ESTATE LYING NOT LESS THAN 500.00 FEET BENEATH THE NATURAL SURFACE, WITHOUT THE RIGHT OF SURFACE ENTRY AS RESERVED BY SANKU FE AND INVESTMENT COMPANY, A CALIFORNIA CORPORATION, IN THE DOCUMENT RECORDED AUGUST 26, 1988 AS INSTRUMENT NO. 244771, OFFICIAL RECORDS.

- EASEMENTS**
- AN EASEMENT AS OFFERED FOR DEDICATION IN FAVOR OF PUBLIC UTILITIES

APN 317-110-088  
 SERRAVALLO  
**JACK IN THE BOX  
 CIRCLE K  
 76 GAS STATION**

**PRELIMINARY EARTHWORK QUANTITIES**  
 CUT: 1,000 CY  
 FILL: 1,000 CY  
 (NON-CALCULATED)

**NOTE:**  
 THE PROPOSED GRADING ON THIS PLAN IS STRICTLY CONCEPTUAL AND SHOULD ONLY BE USED FOR PLANNING PURPOSES.



**SOURCE OF SURVEY**  
 TOPOGRAPHIC SURVEY  
 DATED JULY 2014  
 AS CONDUCTED BY  
**ON POINT LAND SURVEYING, INC.**  
 110 ALVARADO STREET, SUITE 4  
 REDLANDS, CALIFORNIA 92374  
 PHONE: (909) 792-8221  
 FAX: (909) 784-1833

**UTILITIES**

**GAZ:**  
 SOUTHERN CALIFORNIA GAS COMPANY  
 1525 SOUTH SANTA ANITA ROAD  
 HEWLET, CA 92543  
 (951) 943-9484

**ELECTRICITY:**  
 SOUTHERN CALIFORNIA EDISON  
 26100 MENIFEE ROAD  
 BAKERSFIELD, CA 93308  
 PHONE: (805) 838-6545

**SEWER & WATER:**  
 SANITARY MUNICIPAL WATER DISTRICT  
 22701 REYNOLDS ROAD  
 PERRIS, CA 92570  
 PHONE: (909) 928-3777

**TELEPHONE:**  
 VERIZON  
 152 SOUTH SANTA ANITA ROAD  
 HEWLET, CA 92543  
 (951) 943-9484

**CABLE:**  
 TIME WARNER CABLE  
 5207 J. REDBURN STREET  
 LAKE FOREST, CA 92531  
 (909) 611-1344

- LEGEND**
- EX EXISTING
  - PH FIRE HYDRANT
  - PRO PROPOSED
  - RT RIGHT OF WAY
  - SDM STORM DRAIN MANHOLE
  - SL STREET LIGHT
  - SMH SEWER MANHOLE
  - TS TRAFFIC SIGNAL
  - TRM TRAFFIC SIGNAL TALLON
  - EXIST EXISTING STRUCTURE

MARK	REVISIONS	DATE	BY

**BENCHMARK**  
 3" x 4" ALUMINUM DISC SET 1' HIGH IN THE FOOTING OF A STEEL POWER POLE STAMPED: TM 4732, 0.75 INCH W/ET 1" ALONG ORANGE TO PLUM THE B IS 1" FROM TO SEASON AVE. AT THE NORTHEAST CORNER OF CHALICO RD AND SEASON AVE. 42 FEET NORTH OF CHALICO RD AND 30 FEET EAST OF SEASON AVE. ON THE FOOTING OF A LARGE STEEL POWER POLE 705649491 ON LOWER FOOTING 3.8 FEET BELOW UTILITY ELEVATION = 1557.17

**BANS OF BEARING**  
 THE CENTERLINE OF MESSINA LANE PER PMS 21 (01/21/13) BEING 124785.272'

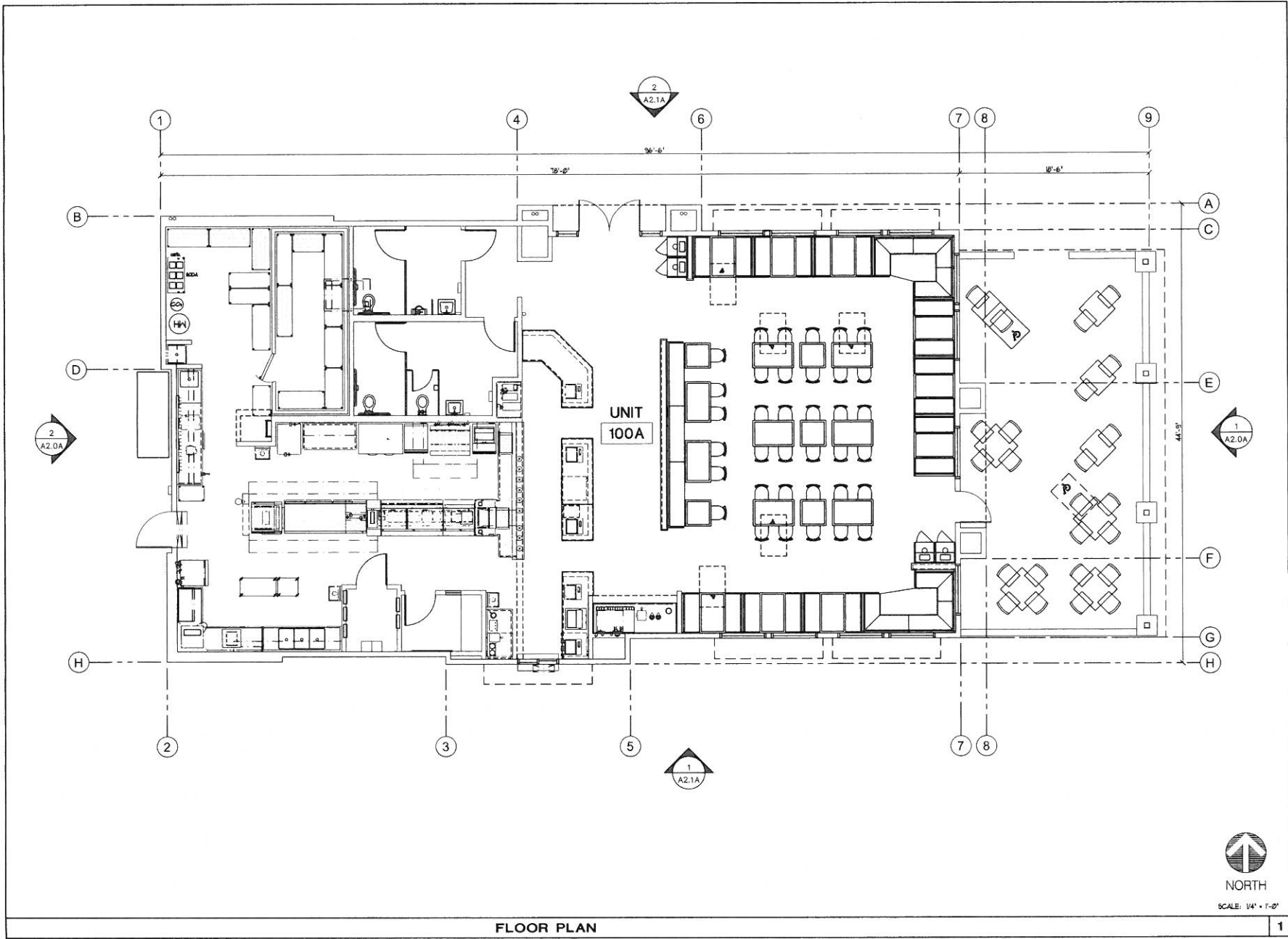
**OWNER:**  
**J.L.S ENTERPRISES, LP, A CALIFORNIA LIMITED PARTNERSHIP**  
 600 THE RIVERSIDE, LLC  
 ATTN: JOE MARCO  
 3800 ORANGE STREET, SUITE 250  
 RIVERSIDE, CA 92501  
 PHONE: (951) 782-8255

**PREPARED FOR/APPLICANT:**  
**HHI RIVERSIDE, LLC**  
 ATTN: JOE MARCO  
 3800 ORANGE STREET, SUITE 250  
 RIVERSIDE, CA 92501  
 PHONE: (951) 782-8255

**EXHIBIT 'A.1'- CONCEPTUAL GRADING PLAN**  
 COUNTY OF RIVERSIDE  
 APN 317-110-057, 058, 070 & 071

**thatcher engineering & associates, inc.**  
 • land planning  
 • civil engineering  
 • landscape architecture  
 1000 W. 17TH STREET  
 ANAHEIM, CA 92801  
 PHONE: (714) 942-1111  
 FAX: (714) 942-1111

DATE PREPARED: 11/20/14  
 DRAWN BY: JF  
 SHEET NUMBER: 13 OF 13  
 REFERENCE NUMBER: 13100000P



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PREPARED BY:  
 HANCOCK ARCHITECTS, INC.  
 1000 CALIFORNIA STREET, SUITE 250  
 SAN FRANCISCO, CA 94109  
 PHONE: (415) 774-4000  
**H&A**  
 ARCHITECTS INC.

PREPARED FOR/APPLICANT:  
 H&A INTERIORS, LLC  
 ATTN: JOE WIND  
 3800 BROADWAY STREET, SUITE 250  
 SAN FRANCISCO, CA 94132  
 PHONE: (415) 774-4000

PROPERTY OWNER:  
 ALLS ENTERPRISES, LP, A  
 CALIFORNIA LIMITED PARTNERSHIP  
 C/O H&A INTERIORS, LLC  
 ATTN: JOE WIND  
 3800 BROADWAY STREET, SUITE 250  
 SAN FRANCISCO, CA 94132  
 PHONE: (415) 774-4000

SHEET TITLE:  
**EXHIBIT "C" - FLOOR PLAN  
 BUILDING "A"**  
 APN: 871-110-057  
 CAJALCO ENTRPRESSWAY 4 HARVILL AVENUE  
 FERRIS, CA

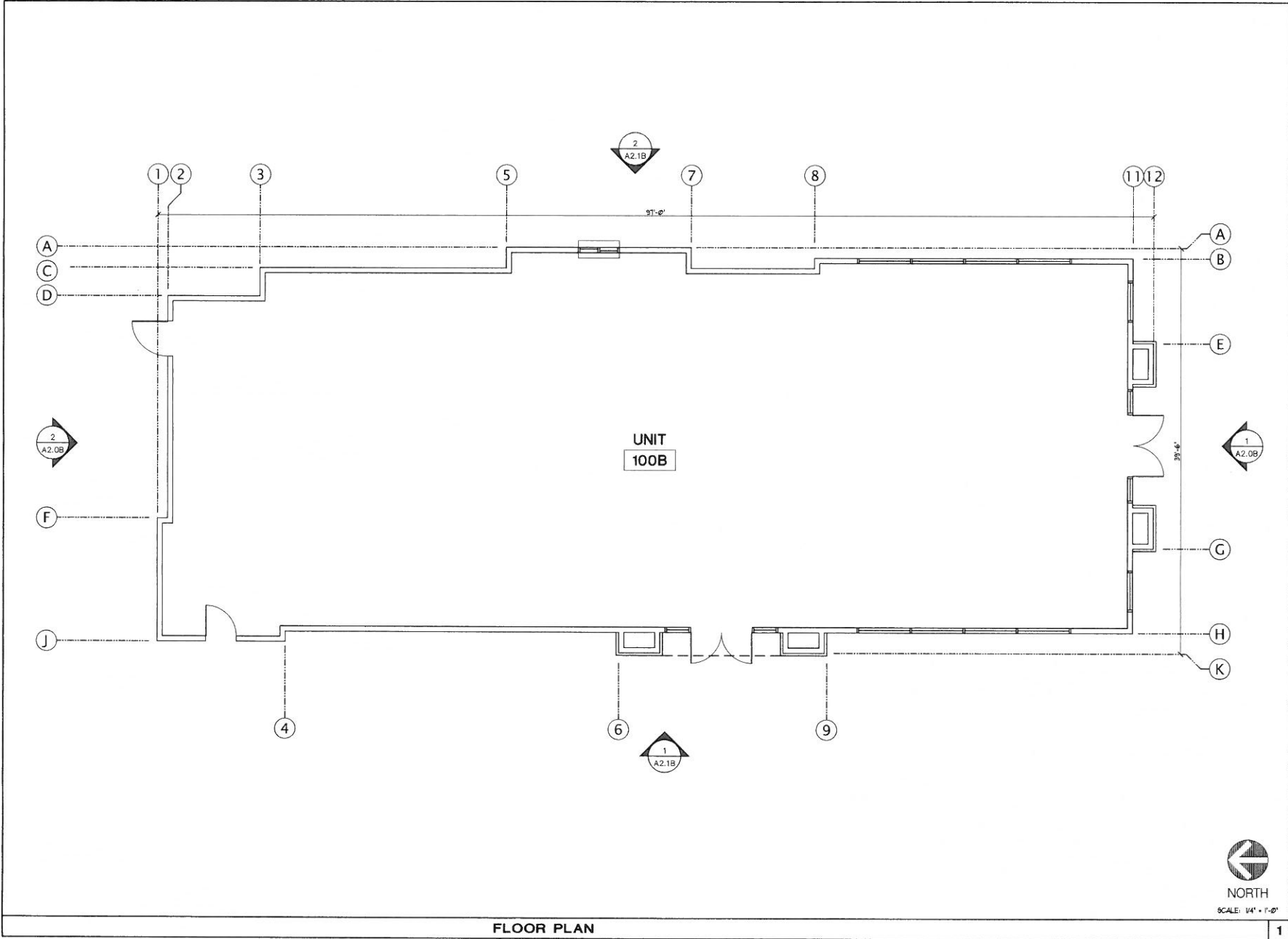
DRAWN BY: P.K.  
 CHECKED BY: D.H.  
 DATE: 11-03-14  
 ISSUE DATE: 11-03-14  
 SCALE: 1/4" = 1'-0"  
 JOB NO: 14026  
 CADD FILE:

SHEET NUMBER:  
**A1.0A**



**FLOOR PLAN**






FLOOR PLAN



SCALE: 1/4" = 1'-0"

1

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PREPARED BY:  
  
 ARCHITECTS, INC.  
 1001 LAWTON LANE  
 SUITE 200  
 FERRIS, CA 94504  
 PHONE: (925) 782-8200

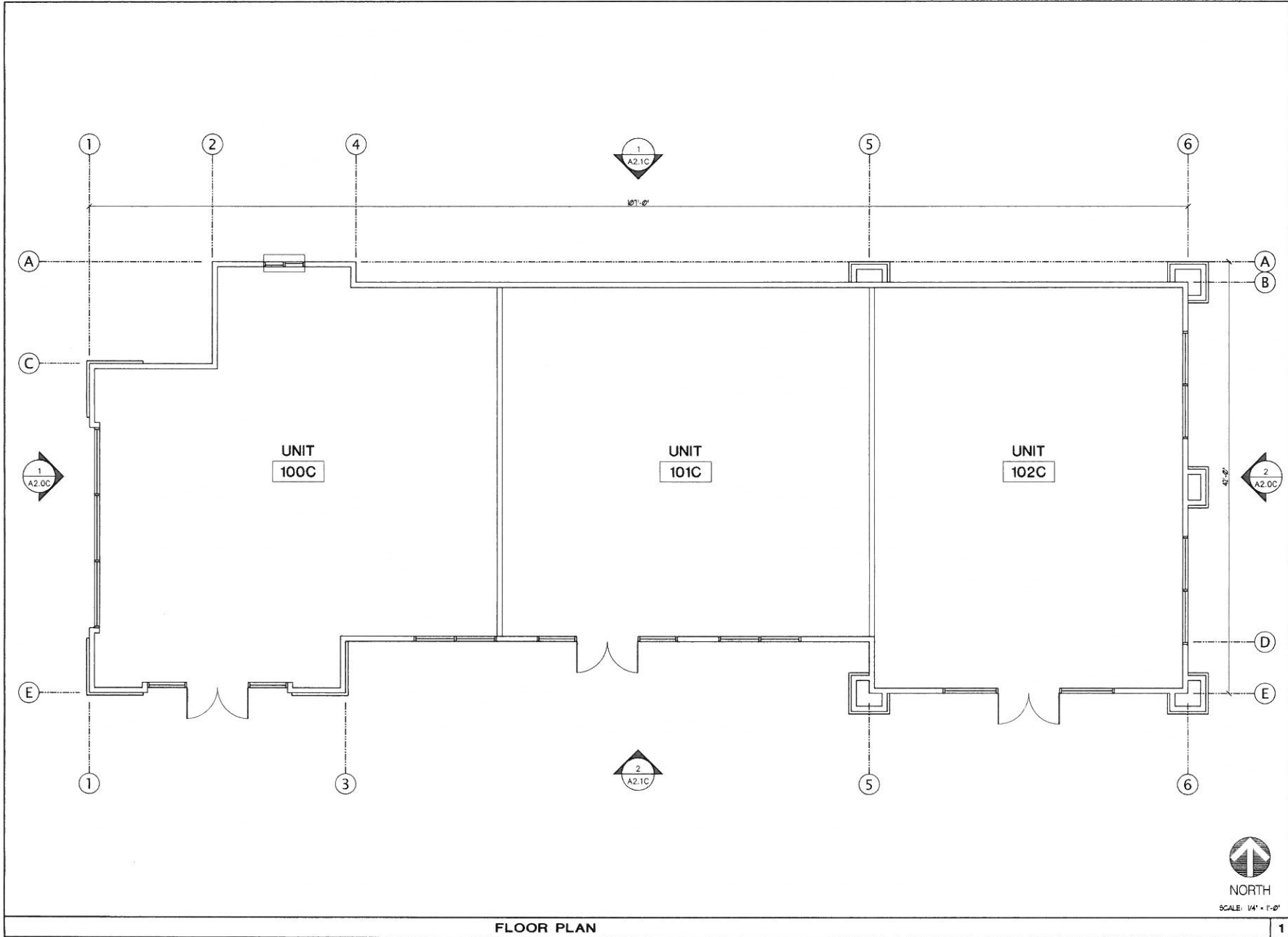
PREPARED FOR/OWNER:  
 THE EXPRESS, LLC  
 ATTN: JOE HINCO  
 3800 ORANGE STREET, SUITE 250  
 FERRIS, CA 94503  
 PHONE: (925) 782-8200

PROPERTY OWNER:  
 JLS ENTERPRISES, LP A  
 CALIFORNIA LIMITED PARTNERSHIP  
 C/O THE EXPRESS, LLC  
 ATTN: JOE HINCO  
 3800 ORANGE STREET, SUITE 250  
 FERRIS, CA 94503  
 PHONE: (925) 782-8200

SHEET TITLE:  
**EXHIBIT "C" - FLOOR PLAN  
 BUILDING "B"**  
 PROJECT:  
 CAJALCO EXPRESSWAY + HARVILL AVENUE  
 FERRIS, CA

DRAWN BY: P.K.  
 CHECKED BY: D.H.  
 DATE: 11-03-14  
 ISSUE DATE: 11-03-14  
 SCALE: 1/4" = 1'-0"  
 JOB NO: 14026  
 CAD FILE:

SHEET NUMBER:  
**A1.0B**



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**HG**  
**ARCHITECTS INC.**

PREPARED FOR APPLICANT:  
**HE WRENCH, LLC**  
 ATTN: JOE WIND  
 3800 ORANGE STREET, SUITE 250  
 FERRIS, CA 94501  
 PHONE: (925)782-8555

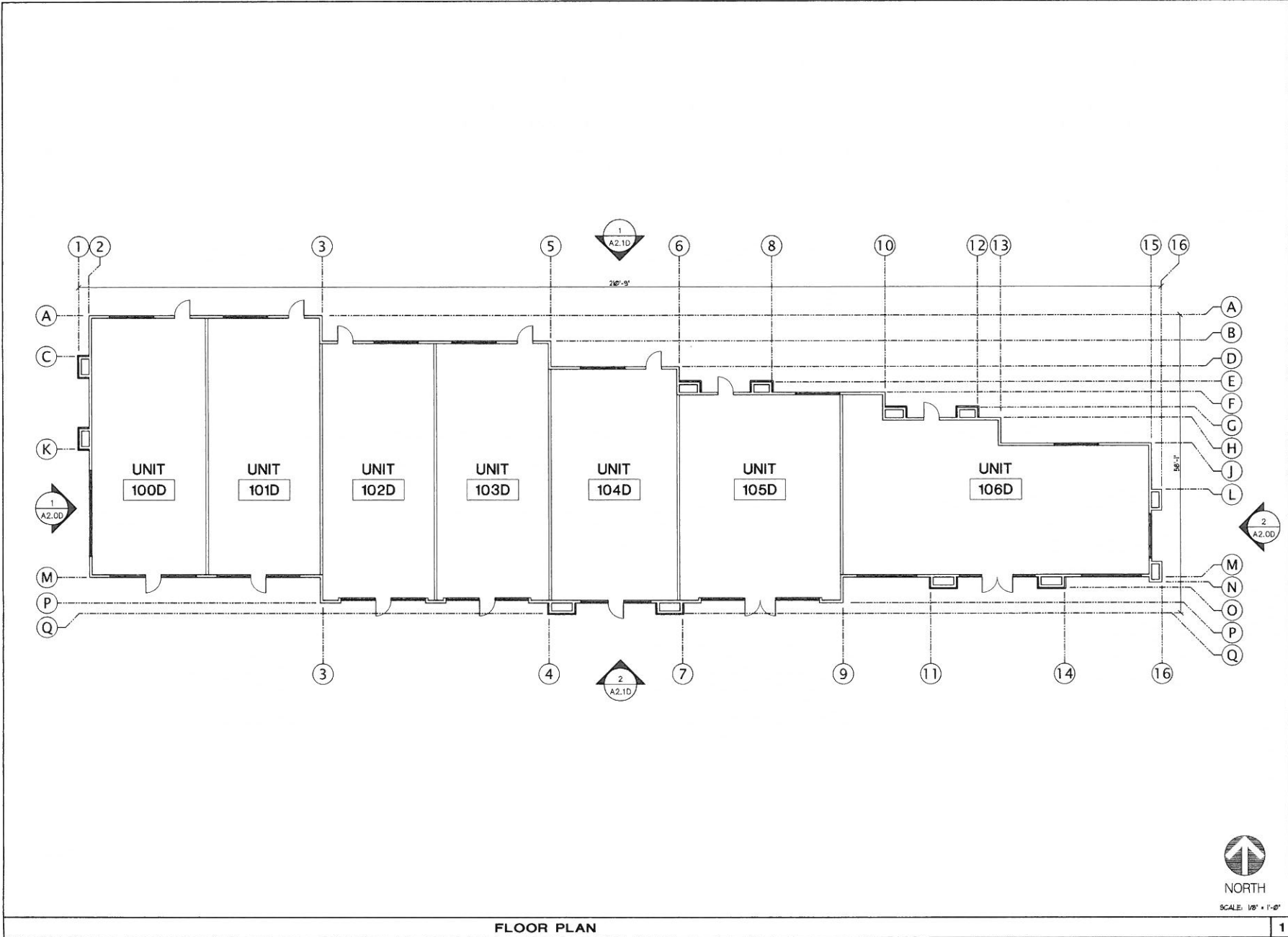
PROJECT OWNER:  
**LLS ENTERPRISES, LP A CALIFORNIA LIMITED PARTNERSHIP**  
 ATTN: JOE WIND  
 3800 ORANGE STREET, SUITE 250  
 FERRIS, CA 94501  
 PHONE: (925)782-8555

SHEET TITLE:  
**EXHIBIT "C" - FLOOR PLAN BUILDING "C"**  
 APN: 877-110-071  
 CAJALCO ENTREPRENEUR 4 MARVILL AVENUE  
 FERRIS, CA

DRAWN BY: P.K.  
 CHECKED BY: D.H.  
 DATE: 11-03-14  
 ISSUE DATE: 11-03-14  
 SCALE: 1/4" = 1'-0"  
 JOB NO: 14026  
 CAD FILE:

SHEET NUMBER:  
**A1.0C**





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PREPARED BY:  
**HCG DESIGN ARCHITECTS, INC.**  
 1000 CALIFORNIA AVENUE  
 SUITE 250  
 PERRIS, CA 92570  
 PHONE: (951)782-8255

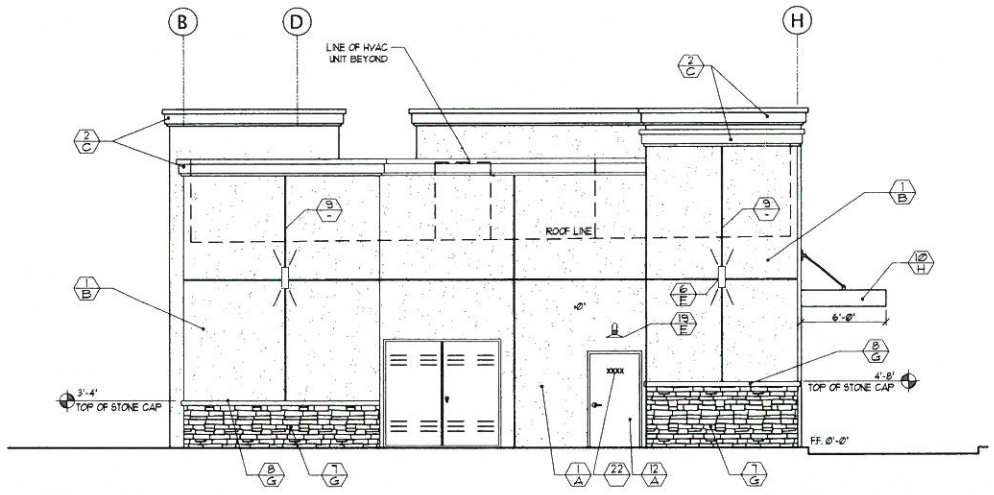
PREPARED FOR/APPLICANT:  
**THE INVERSOLE, LLC**  
 ATTN: JOE WINEY  
 3800 BRANDEGE STREET, SUITE 250  
 PERRIS, CA 92570  
 PHONE: (951)782-8255

PROPERTY OWNER:  
**JLS ENTERPRISES, LP, A CALIFORNIA LIMITED PARTNERSHIP**  
 ATTN: JOE WINEY  
 3800 BRANDEGE STREET, SUITE 250  
 PERRIS, CA 92570  
 PHONE: (951)782-8255

SHEET TITLE:  
**EXHIBIT "C" - FLOOR PLAN BUILDING "D"**  
 APR. 31-11-07-070  
 CAJALCO EXPRESSWAY + HARVILL AVENUE  
 PERRIS, CA

DRAWN BY: P.K.  
 CHECKED BY: D.H.  
 DATE: 11-03-14  
 ISSUE DATE: 11-03-14  
 SCALE: 1/8" = 1'-0"  
 JOB NO: 14026  
 CADD FILE:

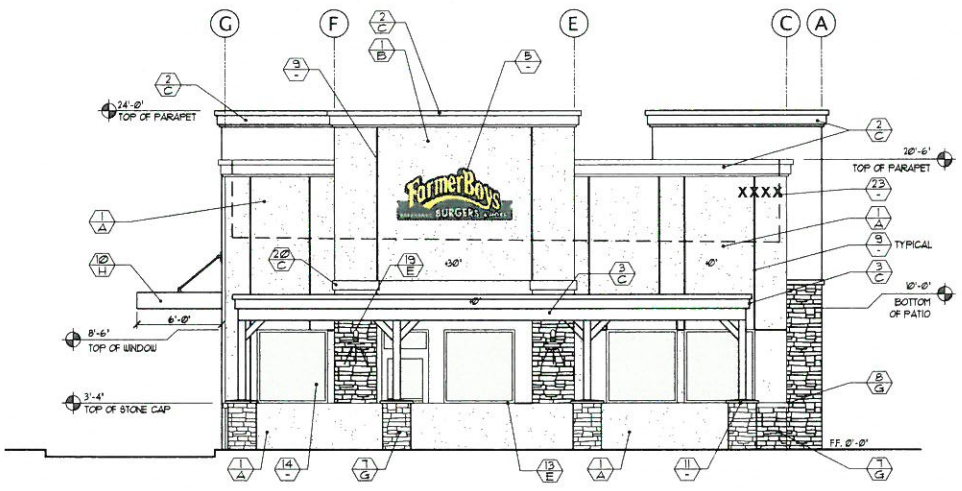
SHEET NUMBER:  
**A1.0D**



WEST ELEVATION

SCALE: 1/4" = 1'-0"

2



EAST ELEVATION

SCALE: 1/4" = 1'-0"

1

KEY NOTES

LEGEND

MATERIAL	FINISH
(Symbol)	(Symbol)

MATERIALS

- 1 PAINT, OVER 3" STUCCO W/MEDIUM SANDED FINISH
- 2 PAINT, OVER CORNICE FOAM TRIM
- 3 WOOD CANOPY FRAMING
- 4 CANVAS AWNING BY SUNBRELLA OR EQ.
- 5 FARMER BOYS SIGNAGE, SEPARATE DESIGN REVIEW APPLICATION REQUIRED
- 6 DECORATIVE UP/DOWN WALL SCONCE HI-LITE MFG MODEL H-1605B-B
- 7 STACK STONE VENEER WAINSCOT
- 8 STONE SILL/WATER TABLE
- 9 GALVANIZED METAL EXPANSION JOINT WITH 1/2" OPENING
- 10 CANTILEVER METAL AWNING
- 11 PREGCAST CONCRETE CAP
- 12 METAL DOOR
- 13 STOREFRONT ALUMINUM FRAMES
- 14 CLEAR WINDOW GLAZING
- 15 PASS-THRU WINDOW WITH STAINLESS STEEL SHELF
- 16 STANDING BEAM METAL ROOF
- 17 GARDEN WALL TRELLIS
- 18 NOT USED
- 19 DECORATIVE DOWN-LIGHT SCONCE HI-LITE MFG MODEL H-9152-B
- 20 ARCHITECTURAL FOAM TRIM
- 21 2X WOOD FASCIA
- 22 4" HIGH VINYL NUMERALS
- 23 12" HIGH x 1 1/2" STROKE INTERNALLY ILLUMINATED ADDRESS SIGN SHALL COMPLY W/ SECTION 5-254 OF ORDINANCE No. 1628 PAGE 16 OF 22

FINISHES

- A SHERWIN WILLIAMS SW680T COMPATIBLE CREAM
- B SHERWIN WILLIAMS SW 1505 SUNDRIED TOMATO
- C SHERWIN WILLIAMS SW6125 CRAFT PAPER
- D SUNBRELLA #4671 (FERN)
- E DARK BRONZE ANODIZED
- F BERRIDGE CEE-LOCK, HEMLOCK GREEN
- G IDAHO DRYSTACK IN CARMEL MOUNTAIN WITH WAINSCOT CAP CHISELED WATER TABLE SILL IN BROWNSTONE
- H SHERWIN WILLIAMS SW1034 STATUS BRONZE

ELEVATION NOTES:

1. CALLOUTS AND KEY NOTES ARE NOT REPEATED FOR ALL ELEVATIONS. TYPICAL ELEMENTS ARE CALLED OUT.
2. BUILDING ADDRESS SHALL BE PROVIDED ON THE BUILDING IN SUCH A POSITION AS TO BE PLAINLY VISIBLE AND LEGIBLE FROM THE STREET FRONTING THE PROPERTY. ALUMINUM ADDRESS NUMBERS 12" HIGH MOUNTED ON EXTERIOR WALL, ILLUMINATED AND CONTRASTING.

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PREPARED BY:  
H2O ARCHITECTS  
ARCHITECTS INC.

PREPARED FOR/APPLICANT:  
H2O ENTERPRISE, LLC  
ATTN: JOE MINDO  
3800 GRANDE STREET, SUITE 250  
MANTOLUCA, CA 95031  
PHONE: (951)782-8255

PROPERTY OWNER:  
H2O ENTERPRISE, LP, A CALIFORNIA LIMITED PARTNERSHIP  
C/O H2O ENTERPRISE, LLC  
ATTN: JOE MINDO  
3800 GRANDE STREET, SUITE 250  
MANTOLUCA, CA 95031  
PHONE: (951)782-8255

SHEET TITLE:  
EXHIBIT "B" - ELEVATIONS BUILDING "A"

DESIGN BY: P.K.  
CHECKED BY: D.H.  
DATE: 11-03-14  
ISSUE DATE: 11-03-14  
SCALE: 1/4" = 1'-0"  
JOB NO.: 14026  
CADD FILE:

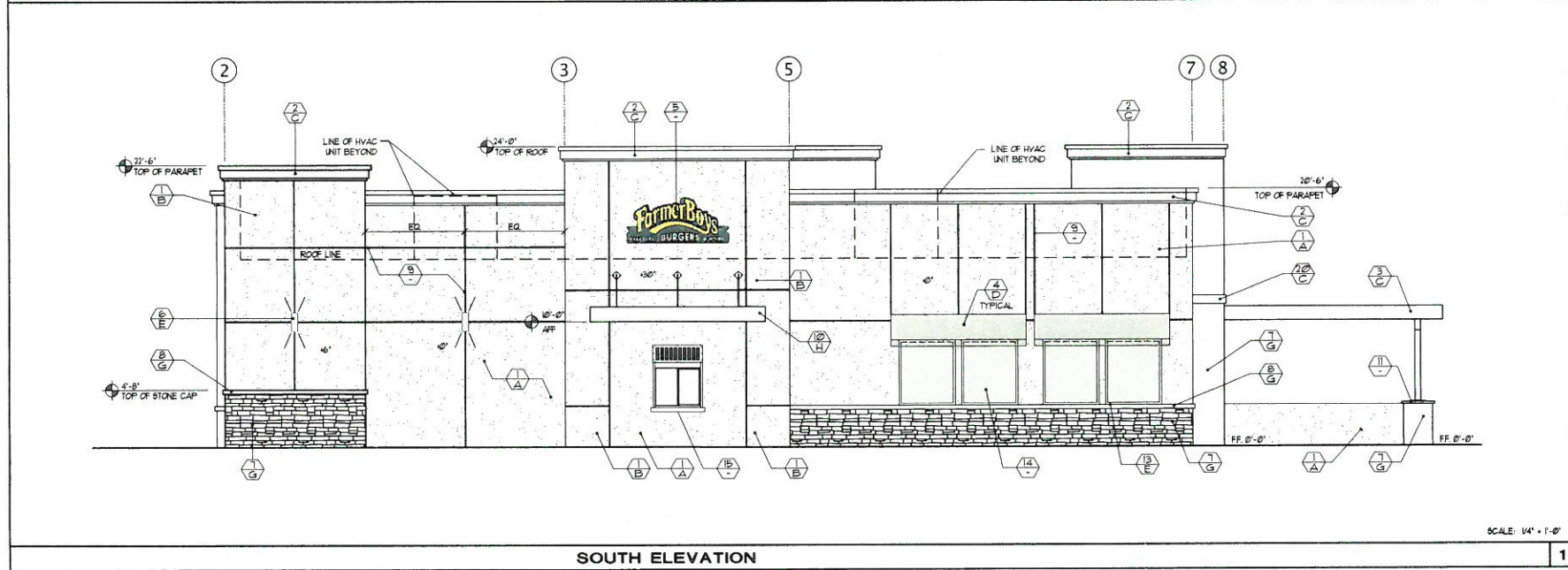
SHEET NUMBER:  
**A2.0A**



NORTH ELEVATION

SCALE: 1/4" = 1'-0"

2



SOUTH ELEVATION

SCALE: 1/4" = 1'-0"

1

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PREPARED BY:  
**H&D**  
ARCHITECTS, INC.

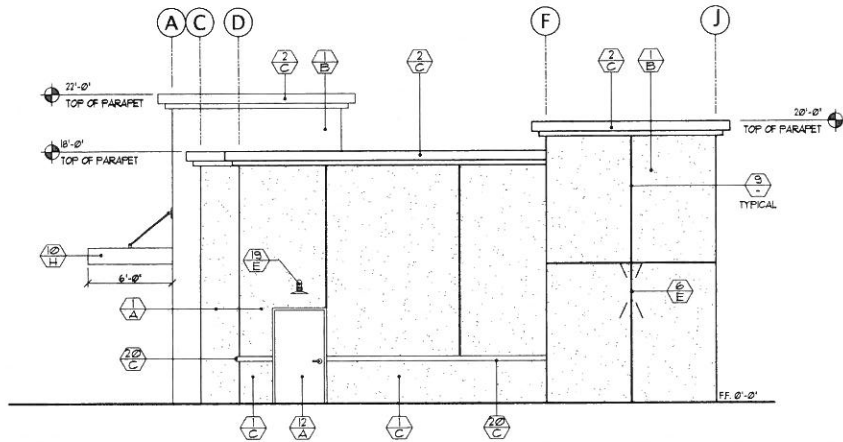
PREPARED FOR APPLICANT:  
**MR. EVERSOLE, LLC**  
ATTN: JOE WINDO  
3000 GRANDE STREET, SUITE 250  
FERRIS, CA 94501  
PHONE: (925) 782-2825

PROJECT OWNER:  
**JLS ENTERPRISES, LP A CALIFORNIA LIMITED PARTNERSHIP**  
C/O MR. EVERSOLE, LLC  
ATTN: JOE WINDO  
3000 GRANDE STREET, SUITE 250  
FERRIS, CA 94501  
PHONE: (925) 782-2825

SHEET TITLE:  
**EXHIBIT "B" - ELEVATIONS BUILDING "A"**  
APN: 271-110-057  
CAJALCO EXPRESSWAY & HARVILL AVENUE  
FERRIS, CA

DRAWN BY: P.K.  
CHECKED BY: D.H.  
DATE: 11-03-14  
ISSUE DATE: 11-03-14  
SCALE: 1/4" = 1'-0"  
JOB NO.: 14026  
CADD FILE:

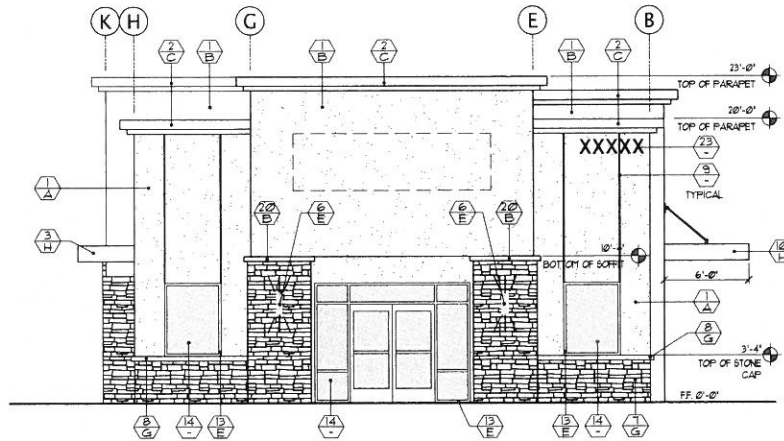
SHEET NUMBER:  
**A2.1A**



SCALE: 1/4" = 1'-0"

2

NORTH ELEVATION



SCALE: 1/4" = 1'-0"

1

SOUTH ELEVATION

KEY NOTES

LEGEND

- (A) MATERIAL
- (B) FINISH

MATERIALS

- (1) PAINT, OVER 3/8" STUCCO W/MEDIUM SANDED FINISH
- (2) PAINT, OVER CORNICE FOAM TRIM
- (3) WOOD CANOPY FRAMING
- (4) CANVAS AWNING BY SUNBRELLA OR EQ.
- (5) FARMER BOYS SIGNAGE, SEPARATE DESIGN REVIEW APPLICATION REQUIRED
- (6) DECORATIVE UP/DOWN WALL SCENCE HI-LITE MFG MODEL H-16038-B
- (7) STACK STONE VENEER HAINSCOT
- (8) STONE SILL/WATER TABLE
- (9) GALVANIZED METAL EXPANSION JOINT WITH 3/8" OPENING
- (10) CANTILEVER METAL AWNING
- (11) PRECAST CONCRETE CAP
- (12) METAL DOOR
- (13) STOREFRONT ALUMINUM FRAMES
- (14) CLEAR WINDOW GLAZING
- (15) PASS-THRU WINDOW WITH STAINLESS STEEL SHELF
- (16) STANDING SEAM METAL ROOF
- (17) GARDEN WALL TRELLIS
- (18) NOT USED
- (19) DECORATIVE DOWN-LIGHT SCENCE HI-LITE MFG MODEL H-41512-B
- (20) ARCHITECTURAL FOAM TRIM
- (21) 2X WOOD FASCIA
- (22) 4" HIGH VINYL NUMERALS
- (23) 12" HIGH x 1 1/2" STROKE INTERNALLY ILLUMINATED ADDRESS SIGN SHALL COMPLY W/ SECTION 5-234 OF ORDINANCE No. 162B PAGE 16 OF 22

FINISHES

- (A) SHERWIN WILLIAMS SW6507 COMPATIBLE CREAM
- (B) SHERWIN WILLIAMS SW 7505 SUNDRIED TOMATO
- (C) SHERWIN WILLIAMS SW6125 CRAFT PAPER
- (D) SUNBRELLA #4671 (FERN)
- (E) DARK BRONZE ANODIZED
- (F) BERRIDGE CEE-LOCK, HEMLOCK GREEN
- (G) IDAHO DRYSTACK IN CARMEL MOUNTAIN WITH HAINSCOT CAP CHISELED WATER TABLE SILL IN BROWNSTONE
- (H) SHERWIN WILLIAMS SK7034 STATUS BRONZE

ELEVATION NOTES:

1. CALLOUTS AND KEY NOTES ARE NOT REPEATED FOR ALL ELEVATIONS. TYPICAL ELEMENTS ARE CALLED OUT.
2. BUILDING ADDRESS SHALL BE PROVIDED ON THE BUILDING IN SUCH A POSITION AS TO BE PLAINLY VISIBLE AND LEGIBLE FROM THE STREET FRONTING THE PROPERTY. ALUMINUM ADDRESS NUMBERS 12" HIGH MOUNTED ON EXTERIOR WALL, ILLUMINATED AND CONTRASTING.

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DESIGNED BY:  
MICHAEL CALVERT ARCHITECTS  
ARCHITECTS INC.  
1000 CALVERT AVENUE  
SAN ANTONIO, TEXAS 78204  
PHONE: (512) 343-1111

PREPARED BY:  
H&B ARCHITECTS  
ARCHITECTS  
1000 CALVERT AVENUE  
SAN ANTONIO, TEXAS 78204  
PHONE: (512) 343-1111

PREPARED FOR/APPLICANT:  
H&B INVERSIDE, LLC  
ATTN: JAC MINDO  
3800 GRANDE STREET, SUITE 250  
MIDLAND, TEXAS 79701  
PHONE: (803) 762-2555

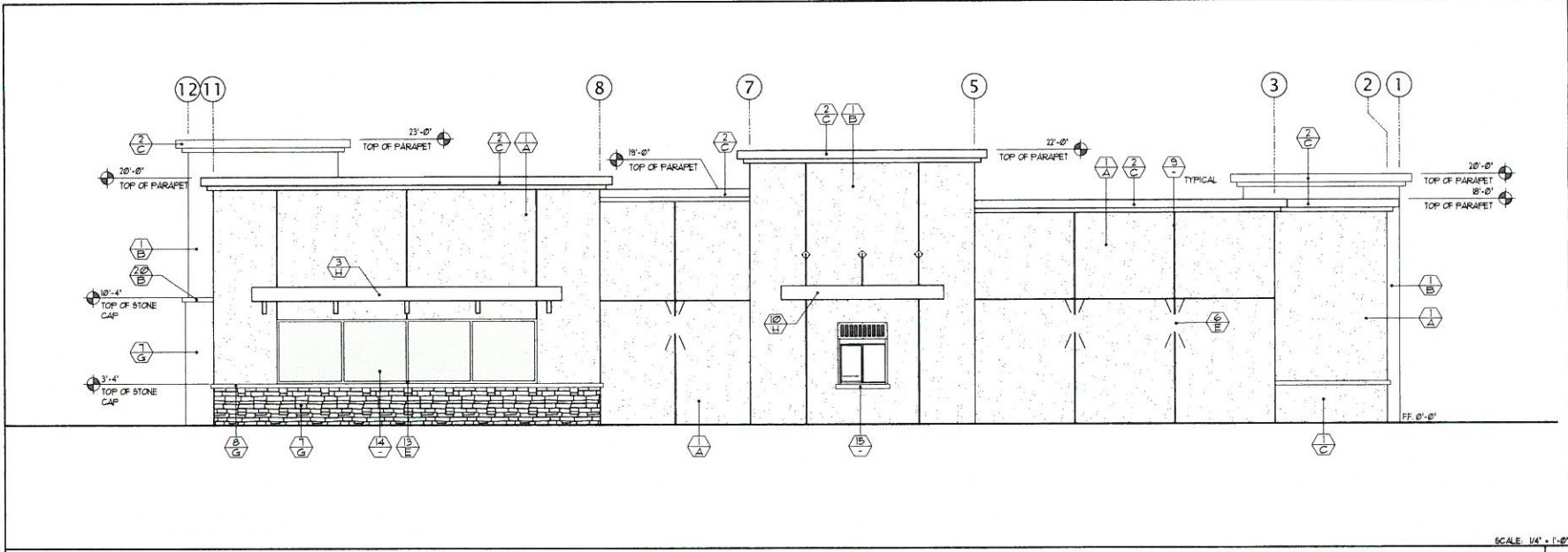
PROPERTY OWNER:  
JLS ENTERPRISES, LP, A  
CALIFORNIA LIMITED PARTNERSHIP  
C/O H&B INVERSIDE, LLC  
ATTN: JAC MINDO  
3800 GRANDE STREET, SUITE 250  
MIDLAND, TEXAS 79701  
PHONE: (803) 762-2555

SHEET TITLE:  
**EXHIBIT "B" - ELEVATIONS**  
**BUILDING "B"**

APN: 871-HIC-056  
CALIFORNIA ENTERPRISES LP & HARVILL AVENUE  
PERRIS, CA

DRAWN BY: P.K.  
CHECKED BY: D.H.  
DATE: 11-03-14  
ISSUE DATE: 11-03-14  
SCALE: 1/4" = 1'-0"  
JOB NO.: 14026  
CADD FILE:

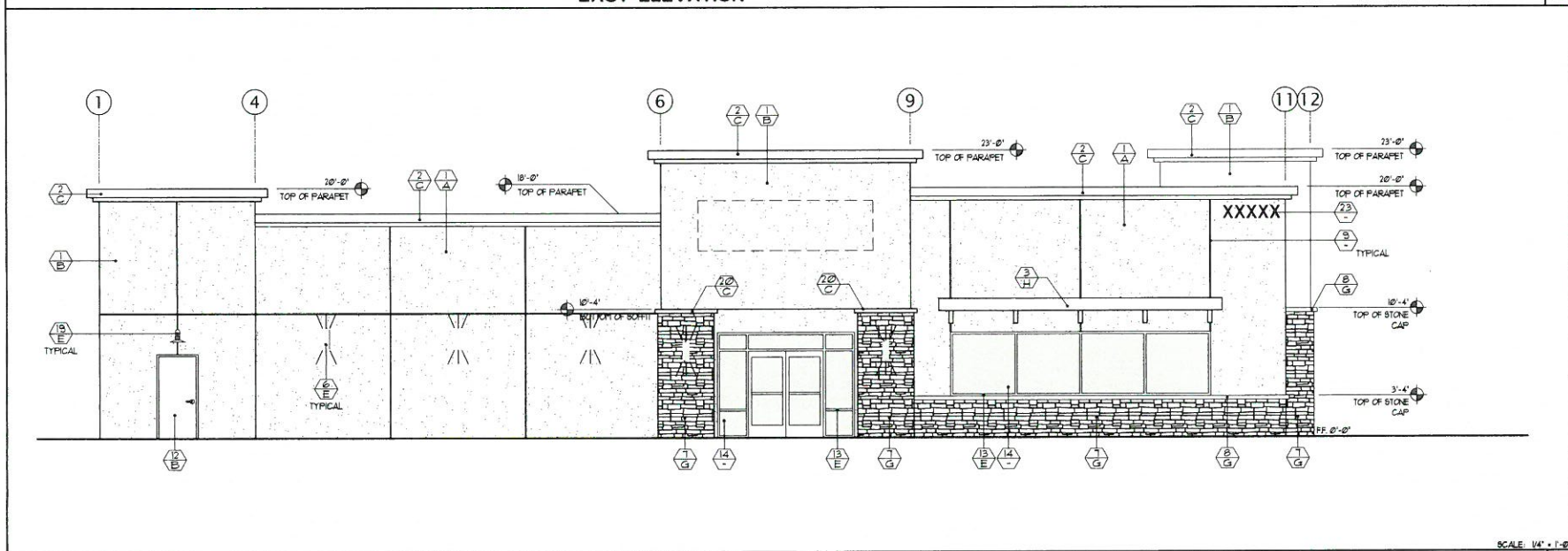
SHEET NUMBER:  
**A2.0B**



EAST ELEVATION

SCALE: 1/4" = 1'-0"

2



WEST ELEVATION

SCALE: 1/4" = 1'-0"

1

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PREPARED BY:  
**H&C ARCHITECTS INC.**  
ARCHITECTS

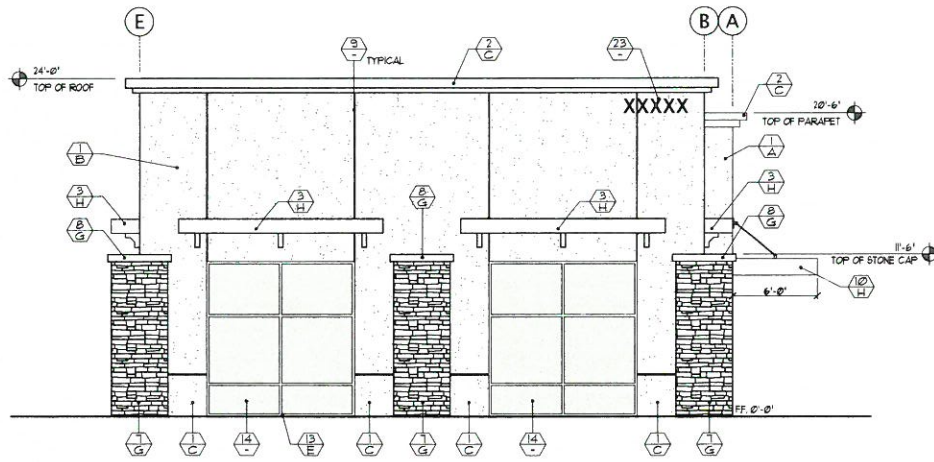
PREPARED (DRAWN/SCALE):  
**H&C INTERIORS, LLC**  
ATTN: JOE WARD  
3600 GRANDE STREET, SUITE 250  
INDIO, CA 92201  
PHONE: (951)782-8255

PROPERTY OWNER:  
**JLS ENTERPRISES, L.P. A CALIFORNIA LIMITED PARTNERSHIP**  
C/O H&C INTERIORS, LLC  
ATTN: JOE WARD  
3600 GRANDE STREET, SUITE 250  
INDIO, CA 92201  
PHONE: (951)782-8255

SHEET TITLE:  
**EXHIBIT "B" - ELEVATIONS BUILDING "B"**  
APN: 871-110-056  
CAJALCO EXPRESSWAY & HARVILL AVENUE  
FERRIS, CA

DRAWN BY: P.K.  
CHECKED BY: D.H.  
DATE: 11-03-14  
ISSUE DATE: 11-03-14  
SHEET NO.: 14026  
GOOD FILE:

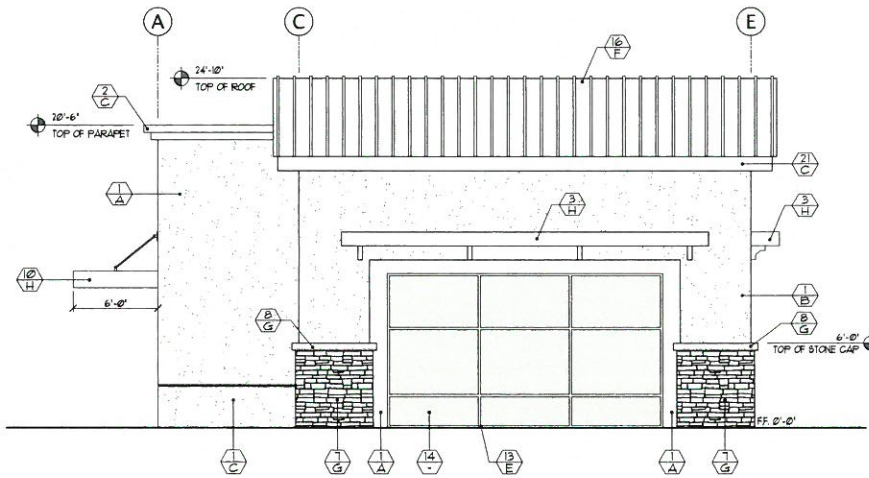
SHEET NUMBER:  
**A2.1B**



EAST ELEVATION

SCALE: 1/4" = 1'-0"

2



WEST ELEVATION

SCALE: 1/4" = 1'-0"

1

KEY NOTES

LEGEND

1 MATERIAL  
2 FINISH

MATERIALS

- 1 PAINT, OVER 3" STUCCO W/MEDIUM SANDED FINISH
- 2 PAINT, OVER CORNICE FOAM TRIM
- 3 WOOD CANOPY FRAMING
- 4 CANVAS AWNING BY SUNBRELLA OR EQ.
- 5 FARMER BOY'S SIGNAGE, SEPARATE DESIGN REVIEW APPLICATION REQUIRED
- 6 DECORATIVE UP/DOWN WALL SCENCE HI-LITE MFS MODEL H-1605D-B
- 7 STACK STONE VENEER WAINSCOT
- 8 STONE SILL/WATER TABLE
- 9 GALVANIZED METAL EXPANSION JOINT WITH 1/2" OPENING
- 10 CANTILEVER METAL AWNING
- 11 PRECAST CONCRETE CAP
- 12 METAL DOOR
- 13 STOREFRONT ALUMINUM FRAMES
- 14 CLEAR WINDOW GLAZING
- 15 PASS-THRU WINDOW WITH STAINLESS STEEL SHELF
- 16 STANDING SEAM METAL ROOF
- 17 GARDEN WALL TRELIS
- 18 NOT USED
- 19 DECORATIVE DOWN-LIGHT SCENCE HI-LITE MFS MODEL H-11512-B
- 20 ARCHITECTURAL FOAM TRIM
- 21 2X WOOD FASCIA
- 22 4" HIGH VINYL NUMERALS
- 23 12" HIGH x 1/2" STROKE INTERNALLY ILLUMINATED ADDRESS SIGN SHALL COMPLY W/ SECTION 5-234 OF ORDINANCE No. 162B PAGE 16 OF 22

FINISHES

- A SHERWIN WILLIAMS SW65BT COMPATIBLE CREAM
- B SHERWIN WILLIAMS SW 7505 SUNDRIED TOMATO
- C SHERWIN WILLIAMS SW6125 CRAFT PAPER
- D SUNBRELLA #4671 (FERN)
- E DARK BRONZE ANODIZED
- F BERRIDGE CEE-LOCK, HEMLOCK GREEN
- G IDAHO DRYSTACK IN CARMEL MOUNTAIN WITH WAINSCOT CAP CHISELED WATER TABLE SILL IN BROWNSTONE
- H SHERWIN WILLIAMS SW7034 STATUS BRONZE

ELEVATION NOTES:

1. CALLOUTS AND KEY NOTES ARE NOT REPEATED FOR ALL ELEVATIONS. TYPICAL ELEMENTS ARE CALLED OUT.
2. BUILDING ADDRESS SHALL BE PROVIDED ON THE BUILDING IN SUCH A POSITION AS TO BE PLAINLY VISIBLE AND LEGIBLE FROM THE STREET FRONTING THE PROPERTY. ALUMINUM ADDRESS NUMBERS 12" HIGH MOUNTED ON EXTERIOR WALL, ILLUMINATED AND CONTRASTING.

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1600 LAVINGTON LANE  
SUITE 100  
SAN JOSE, CA 95128  
PHONE: (408) 991-9999  
FAX: (408) 991-9998  
H&D  
ARCHITECTS, INC.

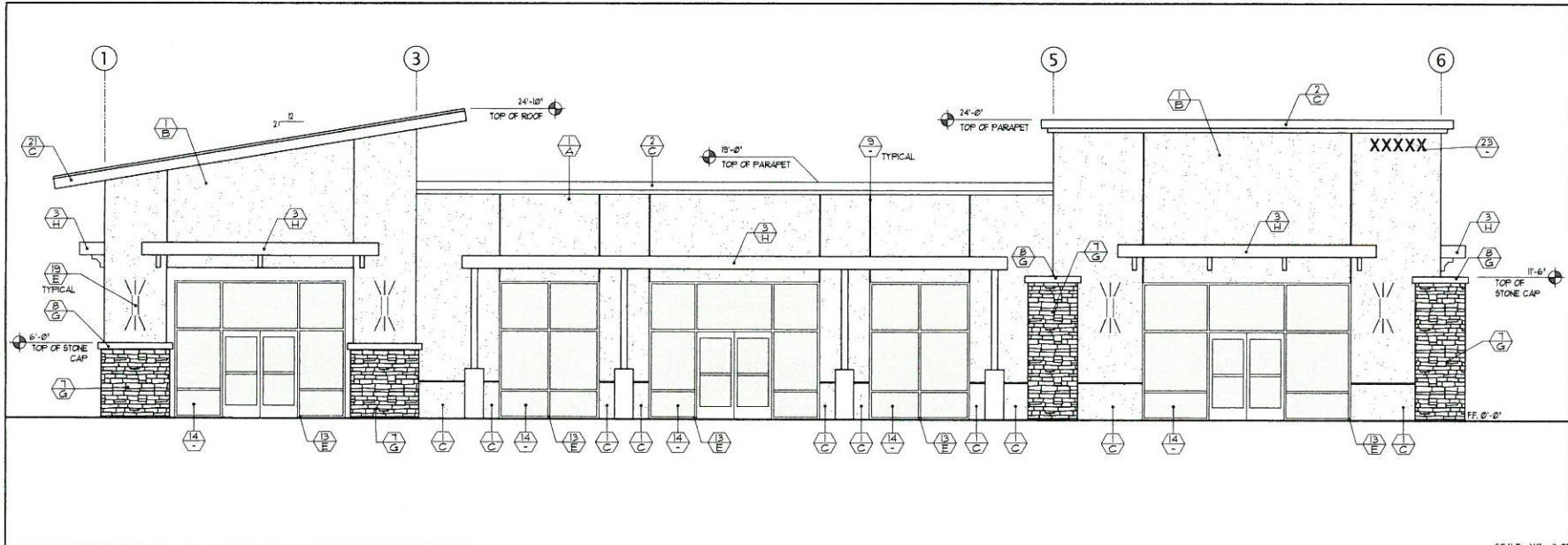
PREPARED BY:  
THE EVERSOLE, LLC  
ATTN: JOE MINO  
3800 GRANGE STREET, SUITE 250  
SAN JOSE, CA 95132  
PHONE: (408) 732-2505

PROPERTY OWNERS:  
JLL ENTERPRISES, LP A  
CALIFORNIA LIMITED PARTNERSHIP  
C/O JH EVERSOLE, LLC  
ATTN: JOE MINO  
3800 GRANGE STREET, SUITE 250  
SAN JOSE, CA 95132  
PHONE: (408) 732-2505

SHEET TITLE:  
EXHIBIT "B" - ELEVATIONS  
BUILDING "C"  
DRAWN BY: P.K.  
CHECKED BY: D.H.  
DATE: 11-03-14  
ISSUE DATE: 11-03-14  
SCALE: 1/4" = 1'-0"  
JOB NO.: 14026  
CADD FILE:  
SHEET NUMBER:  
CAJALCO EXPRESSEWAY & HARVILL AVENUE  
FERRIS, CA

A2.0C

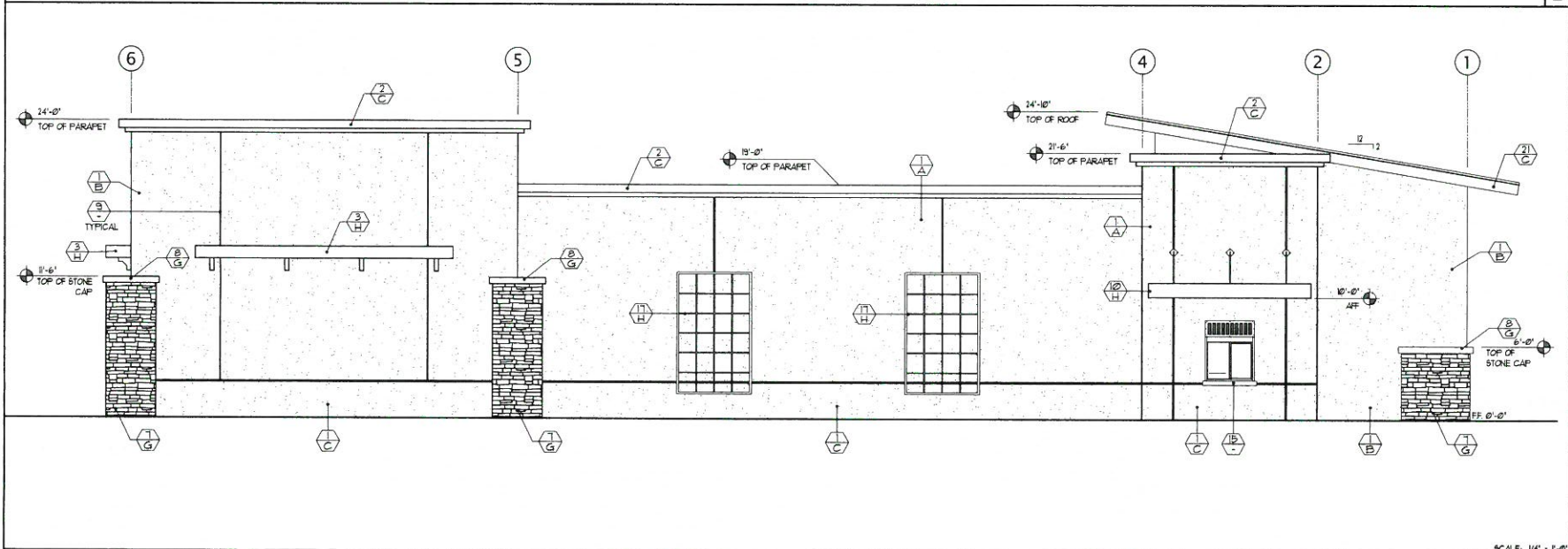




SOUTH ELEVATION

SCALE: 1/4" = 1'-0"

2



NORTH ELEVATION

SCALE: 1/4" = 1'-0"

1

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PREPARED FOR:  
H&B ENGINEERS, LLC  
ARCHITECTS  
3000 GRAND STREET, SUITE 250  
PERDUE, NC 27651  
PHONE: (919) 792-8255

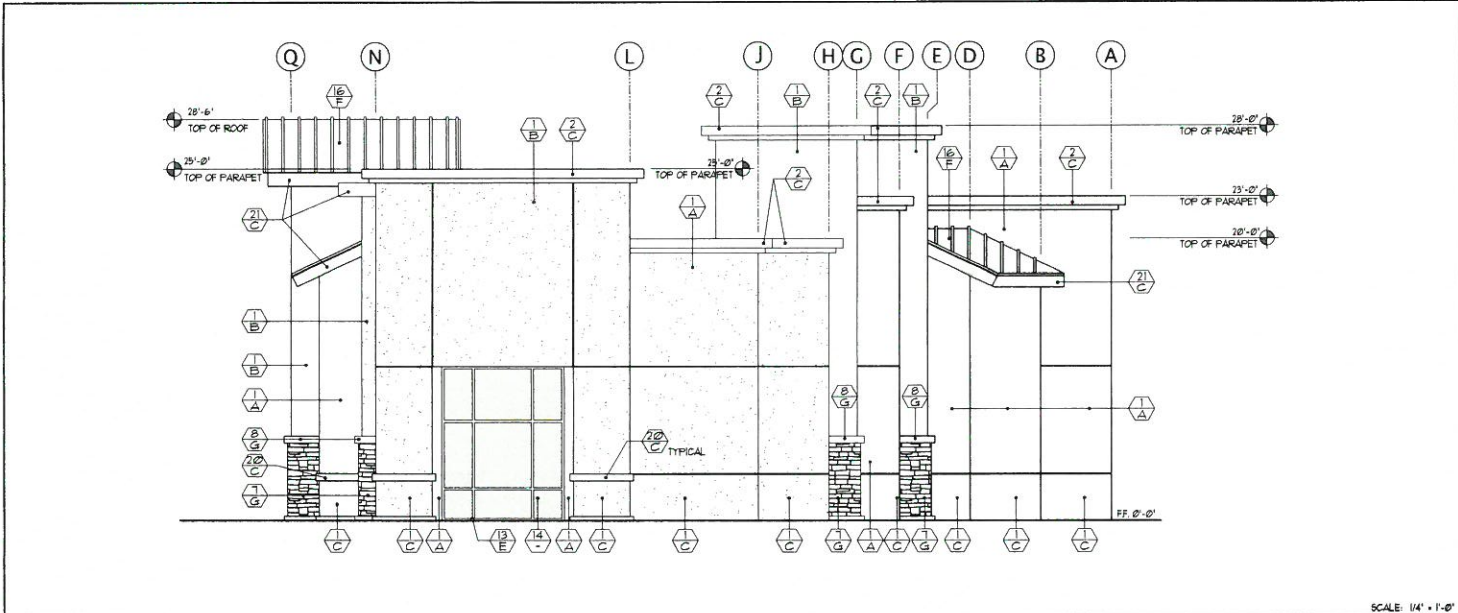
PROPERTY OWNER:  
H&B ENGINEERS, LLC  
ATTN: JOE WINDO  
3000 GRAND STREET, SUITE 250  
PERDUE, NC 27651  
PHONE: (919) 792-8255

PROPERTY OWNER:  
H&B ENGINEERS, LLC  
ATTN: JOE WINDO  
3000 GRAND STREET, SUITE 250  
PERDUE, NC 27651  
PHONE: (919) 792-8255

SHEET TITLE:  
**EXHIBIT "B" - ELEVATIONS  
BUILDING "C"**  
ARCHITECT:  
CAJALCO ENGINEERING & ARCHITECTURE  
PERDUE, NC

DESIGNED BY: P.K.  
CHECKED BY: D.H.  
DATE: 11-03-14  
ISSUE DATE: 11-03-14  
SCALE: 1/4" = 1'-0"  
JOB NO: 14026  
CADD FILE:

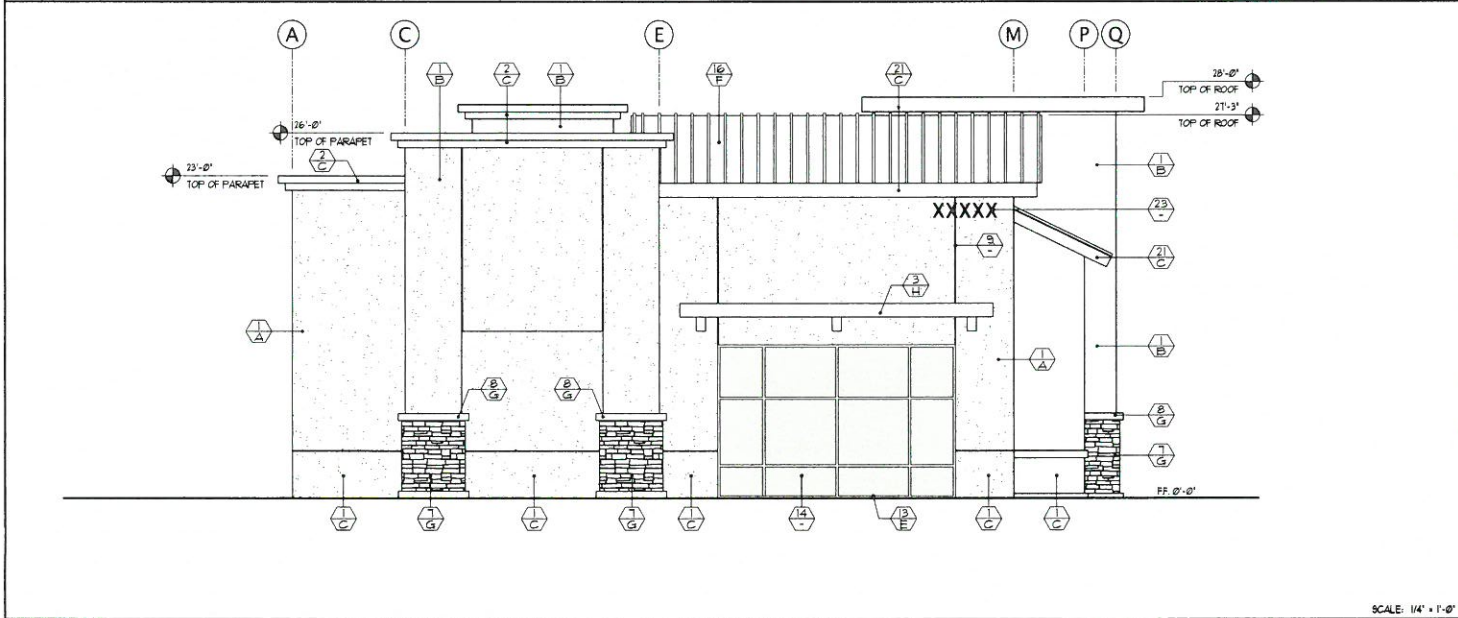
SHEET NUMBER:  
**A2.1C**



EAST ELEVATION

SCALE: 1/4" = 1'-0"

2



WEST ELEVATION

SCALE: 1/4" = 1'-0"

1

KEY NOTES	
<b>LEGEND</b>	
	MATERIAL
	FINISH
<b>MATERIALS</b>	
	PAINT, OVER 1/2" STUCCO W/MEDIUM SANDED FINISH
	PAINT, OVER CORNICE FOAM TRIM
	WOOD CANOPY FRAMING
	CANVAS AWNINGS BY SUNBRELLA OR EQ.
	FARMER BOYS SIGNAGE SEPARATE DESIGN REVIEW APPLICATION REQUIRED
	DECORATIVE UP/DOWN WALL SCONCE H-LITE MFG MODEL H-1605D-B
	STACK STONE VENEER WAINSCOT
	STONE SILL/WATER TABLE
	GALVANIZED METAL EXPANSION JOINT WITH 1/2" OPENING
	CANTILEVER METAL AWNING
	PRECAST CONCRETE CAP
	METAL DOOR
	STOREFRONT ALUMINUM FRAMES
	CLEAR WINDOW GLAZING
	PASS-THRU WINDOW WITH STAINLESS STEEL SHELF
	STANDING SEAM METAL ROOF
	GARDEN WALL TRELIS
	NOT USED
	DECORATIVE DOWN-LIGHT SCONCE H-LITE MFG MODEL H-41512-B
	ARCHITECTURAL FOAM TRIM
	2X WOOD FASCIA
	4" HIGH VINYL NUMERALS
	12" HIGH x 1/4" STROKE INTERNALLY ILLUMINATED ADDRESS SIGN SHALL COMPLY W/ SECTION 5-2B4 OF ORDINANCE No. 162B PAGE 16 OF 22
<b>FINISHES</b>	
	SHERWIN WILLIAMS SW630T COMPATIBLE CREAM
	SHERWIN WILLIAMS SW 7505 SUNDRIED TOMATO
	SHERWIN WILLIAMS SW6125 CRAFT PAPER
	SUNBRELLA #4671 (FERN)
	DARK BRONZE ANODIZED
	BERRIDGE CEE-LOCK, HEMLOCK GREEN
	IDAHO DRYSTACK IN CARMEL MOUNTAIN WITH WAINSCOT CAP CHISELED WATER TABLE SILL IN BROWNSTONE
	SHERWIN WILLIAMS SW2034 STATUS BRONZE
<b>ELEVATION NOTES:</b>	
1. CALLOUTS AND KEY NOTES ARE NOT REPEATED FOR ALL ELEVATIONS. TYPICAL ELEMENTS ARE CALLED OUT.	
2. BUILDING ADDRESS SHALL BE PROVIDED ON THE BUILDING IN SUCH A POSITION AS TO BE PLAINLY VISIBLE AND LEGIBLE FROM THE STREET FRONTING THE PROPERTY ALUMINUM ADDRESS NUMBERS 12" HIGH MOUNTED ON EXTERIOR WALL, ILLUMINATED AND CONTRASTING.	

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**H&D ARCHITECTS INC.**  
ARCHITECTS

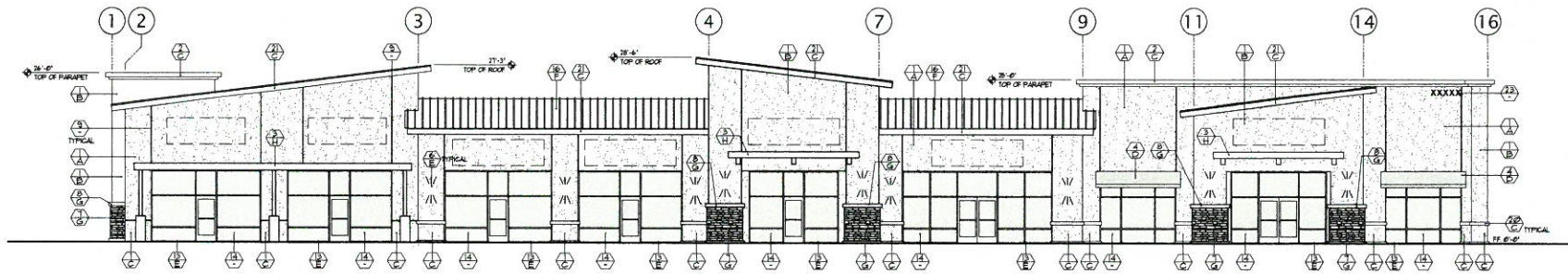
PREPARED FOR APPLICANT:  
**HE INVERSIDE, LLC**  
ATTN: JOE WIND  
3800 GRANDE STREET, SUITE 200  
SAN JOSE, CA 95134  
PHONE: (408) 732-2255

PROPERTY OWNER:  
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C/O HE INVERSIDE, LLC  
ATTN: JOE WIND  
3800 GRANDE STREET, SUITE 200  
SAN JOSE, CA 95134  
PHONE: (408) 732-2255

SHEET TITLE:  
**EXHIBIT "B" - ELEVATIONS BUILDING "D"**  
APN: 397-110-070  
CALIFORNIA ENTERPRESNAY & MARYVILL AVENUE  
FERRIS, CA

DRAWN BY: P.K.  
CHECKED BY: D.H.  
DATE: 11-03-14  
ISSUE DATE: 11-03-14  
SCALE: 1/4" = 1'-0"  
JOB NO.: 14026  
CAD FILE:

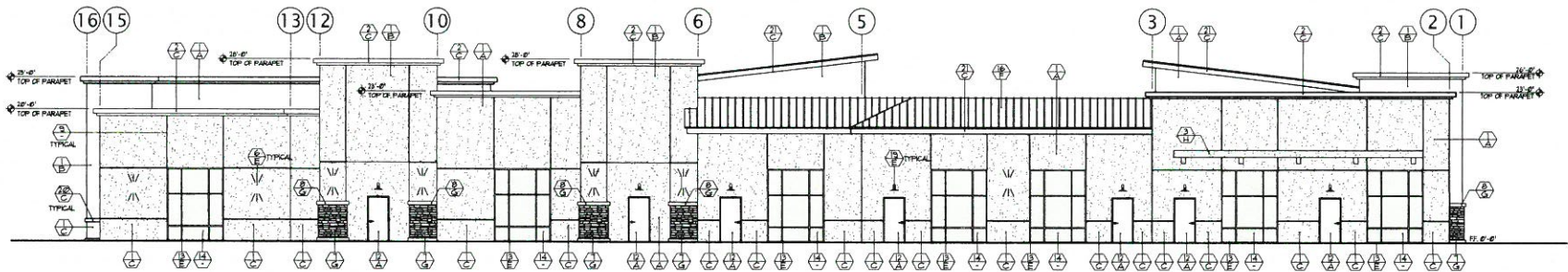
SHEET NUMBER:  
**A2.0D**



SOUTH ELEVATION

SCALE: 1/8" = 1'-0"

2



NORTH ELEVATION

SCALE: 1/8" = 1'-0"

1

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DESIGNED BY:  
H&D ARCHITECTURE, INC.  
3000 ORANGE STREET, SUITE 250  
IRVINE, CA 92614  
PHONE: (949) 714-8225  
FAX: (949) 714-8224

PREPARED FOR APPLICANT:  
H&D INVESTMENT, LLC  
3000 ORANGE STREET, SUITE 250  
IRVINE, CA 92614  
PHONE: (949) 714-8225

PROPERTY OWNER:  
H&D INVESTMENT, LLC  
CALIFORNIA LIMITED PARTNERSHIP  
3000 ORANGE STREET, SUITE 250  
IRVINE, CA 92614  
PHONE: (949) 714-8225

SHEET TITLE:  
EXHIBIT "B" - ELEVATIONS  
BUILDING "D"  
APN: 97-10-070  
CAJALCO EXPRESSWAY & HARVILL AVENUE  
FERRIS, CA

DRAWN BY: P.K.  
CHECKED BY: D.H.  
DATE: 11-03-14  
ISSUE DATE: 11-03-14  
SCALE: 1/4" = 1'-0"  
JOB NO: 14026  
CAD FILE:

SHEET NUMBER:  
**A2.1D**

# NOTICE OF PUBLIC HEARING

## RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14<sup>th</sup> Floor, Riverside, California 92501, Monday through Thursday, from 8:00 a.m. to 5:00 p.m., and by prescheduled appointment on Fridays, from 9:00 a.m. to 5:00 p.m.

PLACE OF HEARING: Riverside County Administration Center  
4080 Lemon St., 1<sup>st</sup> Floor Hearing Room  
Riverside, California

DATE OF HEARING: April 9, 2015

TIME OF HEARING: 9:00 A.M.

### CASE DESCRIPTION:

ZAP1111MA15 – HHI Riverside, LLC (Representative: Joe Mineo) – County Case Nos. PP25699 (Plot Plan) and VAR 01893 (Variance). The applicant proposes to develop a 19,558 square foot retail and dining center on 3.42 net acres (5.06 gross acres) located at the northeasterly corner of Cajalco Expressway and Harvill Avenue in the unincorporated community of Mead Valley. Plot Plan No. 25699 would authorize development of two retail commercial buildings with a total floor area of 12,872 square feet (one of which would include a 1,440 square foot coffee shop served by a drive-thru lane) and two freestanding restaurants with drive-thru (3,434 and 3,252 square feet, respectively, one of which would be a Farmer Boys). Variance Case No. 1893 is a proposal to exceed the allowable number, height, and surface area of on-site advertising signs. Specifically, the applicant proposes a pylon sign 75 feet in height with a total display area of 540 square feet, in addition to two freestanding monument signs. (Airport Compatibility Zone C2 of the March Air Reserve Base/Inland Port Airport Influence Area).

FURTHER INFORMATION: Contact Russell Brady at (951) 955-0549 or John Guerin at (951) 955-0982. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Ms. Bahelila Boothe of the County of Riverside Planning Department, at (951) 955-8703.

**APPLICATION FOR MAJOR LAND USE ACTION REVIEW**  
**RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**

ALUC Identification No.

ZAP1111MA15

**PROJECT PROPONENT (TO BE COMPLETED BY APPLICANT)**

Date of Application \_\_\_\_\_

Property Owner HHI Riverside, LLC.

Phone Number 951-782-8255

Mailing Address 3800 Orange St. #250, Riverside, Calif. 92501

Agent (if any) Joe Mineo

Phone Number 951-782-8255

Mailing Address 3800 Orange St. #250, Riverside, Calif. 92501

**PROJECT LOCATION (TO BE COMPLETED BY APPLICANT)**

*Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways*

Street Address \_\_\_\_\_

Assessor's Parcel No. 317-110-057, 317-110-058, 357-110-070, 317-110-071 Parcel Size Total = 3.4 acre

Subdivision Name \_\_\_\_\_

Zoning CPS

Lot Number \_\_\_\_\_

Classification Scenic Hwy Commercial

**PROJECT DESCRIPTION (TO BE COMPLETED BY APPLICANT)**

*If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed*

Existing Land Use (describe) Vacant Undeveloped Land

Proposed Land Use (describe) Proposed shopping center -phased- 3.4 acres to include 2 drive-thru restaurants, 2 retail commercial buildings, 49,211s/f of landscaping, 158 parking spaces, 2 monument & 1 pylon sign 75 feet high. Total GLA = 20,111 s/f. On June 8, 2006 ALUC approved a similar project under File #MA-06-120, related file CUP 03468

For Residential Uses Number of Parcels or Units on Site (exclude secondary units) \_\_\_\_\_

For Other Land Uses Hours of Use \_\_\_\_\_

(See Appendix C) Number of People on Site Maximum Number \_\_\_\_\_

Method of Calculation \_\_\_\_\_

Height Data Height above Ground or Tallest Object (including antennas and trees) Pylon sign 75 feet ft.

Highest Elevation (above sea level) of Any Object or Terrain on Site 1596 ft.

Flight Hazards Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?  Yes  No

If yes, describe \_\_\_\_\_

March  
C2

**REFERRING AGENCY** (APPLICANT OR JURISDICTION TO COMPLETE)

Date Received \_\_\_\_\_

Agency Name \_\_\_\_\_

Staff Contact \_\_\_\_\_

Phone Number \_\_\_\_\_

Agency's Project No. \_\_\_\_\_

County of Riverside

Bahcila Boothe

PP25699 / VAR 01893

Type of Project

- General Plan Amendment
- Zoning Amendment or Variance
- Subdivision Approval
- Use Permit
- Public Facility
- Other \_\_\_\_\_

**A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

**B. SUBMISSION PACKAGE:**

**ALUC REVIEW**

- 1 . . . . . Completed Application Form
- 1 . . . . . Project Site Plan – Folded (8-1/2 x 14 max.)
- 1 . . . . . Elevations of Buildings - Folded
- 1 Each . 8 ½ x 11 reduced copy of the above
- 1 . . . . . 8 ½ x 11 reduced copy showing project in relationship to airport.
- 1 Set . Floor plans for non-residential projects
- 4 Sets. . Gummed address labels of the Owner and representative (**See Proponent**).
- 1 Set. . Gummed address labels of all property owners within a 300' radius of the project site. If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10), with ALUC return address.
- 4 Sets. . Gummed address labels of the referring agency (City or County).
- 1 . . . . . Check for Fee (See Item "C" below)

**STAFF REVIEW (Consult with ALUC staff planner as to whether project qualifies)**

- 1 . . . . . Completed Application Form
- 1 . . . . . Project Site Plans – Folded (8-1/2 x 14 max.)
- 1 . . . . . Elevations of Buildings - Folded
- 1 . . . . . 8 ½ x 11 Vicinity Map
- 1 Set . Gummed address labels of the Owner and representative (**See Proponent**).
- 1 Set . Gummed address labels of the referring agency.
- 1 . . . . . Check for review–See Below



- land planning
- civil engineering
- landscape architecture

phone 909.748.7777  
 fax 909.748.7776

**thatcher engineering & associates, inc.**  
 1461 ford street, suite 105, redlands, ca 92373

**Transmittal**

**To:** County of Riverside  
 Airport Land Use Commission  
 4080 Lemon Street, 14<sup>th</sup> Floor  
 Riverside, CA 92502

**Date:** February 17, 2015  
**Job No.:** 151806

**Project:** APN 317-110-057, 058, 070, and 071, *Northeast corner of Cajalco Expy. & Harville Ave. (Perris Area)*, **PP25699 and VAR01893**

**We are sending you**     Attached     Under Separate Cover via \_\_\_\_\_ the following items:

<b>Copies</b>	<b>Date</b>	<b>No.</b>	<b>Description</b>
1			Completed Application for Major Land Use Action Review with Project Location Exhibit
1			Copy of Site Plan, Floor Plans, and Building Elevations
1			Reduced copies of Site Plan, Floor Plan, and Building Elevations
1			Surrounding Property Owner Label Package: <ul style="list-style-type: none"> <li>• 300 ft. Radius Map</li> <li>• 4 Sets of Owner, Representative, and Referring Agency Labels</li> <li>• 1 Set of Surrounding Property Owners</li> </ul>
1			Copy of Previous Approval Letter dated 7/10/2006 and Approved Site Plan ( <i>For Reference</i> )
1			Check for the payment of the Application Review Fee

**Remarks:**

For review and approval.

Copy to:

Signed: \_\_\_\_\_  
 Vicky Valenzuela, Project Manager

COUNTY OF RIVERSIDE  
AIRPORT LAND USE COMMISSION  
STAFF REPORT

**AGENDA ITEM:** 3.2

**HEARING DATE:** April 9, 2015

**CASE NUMBER:** ZAP1025PS15 – PS Country Club, LLC, Eric Taylor (Representative: MSA Consulting, Marvin Roos)

**APPROVING JURISDICTION:** City of Palm Springs

**JURISDICTION CASE NO.:** TTM36691 (Tentative Tract Map), Case 5.1327 (General Plan Amendment)

**MAJOR ISSUES:** The project meets the overall open area requirement as a result of an open space remainder lot to be preserved as part of the Whitewater Wash. However, the open area is located within Compatibility Zone D. The project is technically not compliant with the requirement for open area in Zone C, but the amount of open space overall is well above the minimum level required.

**RECOMMENDATION:** Staff recommends a finding of CONSISTENCY for the General Plan Amendment and CONDITIONAL CONSISTENCY for the Tentative Tract Map.

**PROJECT DESCRIPTION:**

Tentative Tract Map No. 36691 would divide 131.25 acres located northerly of Verona Road, easterly of Sunrise Way, and southwesterly of the Whitewater River Channel into 429 residential lots (292 detached units and 137 attached units), 5.39 acres of public open space, and 25 private open space lots. Case No. 5.1327 is a proposal to amend the General Plan designation of 125.86 acres from Open Space-Parks/Recreation (OS-P/R) to Very Low Density Residential (up to 4.0 dwelling units per acre) (VLDR). PD-366 is a proposal to place the same 125.86 acres in a Planned Development District, superseding the existing O and O5 open space zoning.

The 137 attached single-family lots would encircle the existing Golden Sands Mobile Home Park accessed via Golden Sands Drive, easterly from Sunrise Way. The 292 detached single-family lots would encircle a residential neighborhood accessed via Whitewater Club Drive, easterly from Farrell Drive.

**PROJECT LOCATION:**



The project site is located easterly of Sunrise Way, southerly of Four Seasons Boulevard, westerly of Gene Autry Trail, and northerly of Racquet Club Road, and Verona Road. At its closest point, the project site is located approximately 4,320 feet northerly of the northerly end of Runway 13R-31L at Palm Springs International Airport.

**LAND USE PLAN:** 2005 Palm Springs International Airport Land Use Compatibility Plan

- a. Airport Influence Area: Palm Springs International Airport
- b. Land Use Policy: Compatibility Zones C and D
- c. Noise Levels: Below 60 CNEL from aircraft noise

Land Use/Density: The proposed project is located within Compatibility Zones C and D, with the northwest portion of the project within Compatibility Zone C and the southeast portion of the project primarily within Compatibility Zone D. Within this Airport Influence Area, Additional Compatibility Policy Nos. 2.2 and 2.3 of the Palm Springs International Airport Land Use Compatibility Plan allow residential densities of 3-15 dwelling units per acre in both Compatibility Zones C and D.

The Compatibility Plan includes Additional Compatibility Policies requiring residential projects in the Airport Influence Area (except within Compatibility Zone E) to implement expanded buyer awareness measures. Specifically, the developer must install large airport-related informational signs during the initial sales period and provide prospective buyers and renters with informational brochures.

Overall, based on the gross acreage of the property (156.18 acres), the Tentative Tract Map proposes a density of 2.74 dwelling units per acre. However, the Palm Springs International Airport Land Use Compatibility Plan (PSIALUCP) allows for residential density to be calculated on a net basis, pursuant to Countywide Policies Table 2A Footnote 16 as amended by RG-05-103. When the 35.62-acre remainder lot and lot W (a natural open area being preserved as part of the Whitewater wash area) is excluded, the net area is reduced to 120.56 acres. Based on this net acreage, the project proposes an overall density of 3.55 dwelling units per acre. This density would meet the Compatibility Zone C and D criteria of a minimum of 3.0 dwelling units per acre.

Looking at the net acreages within each respective Compatibility Zone, approximately 36.65 net acres are located within Compatibility Zone C, and approximately 83.91 net acres are located within Compatibility Zone D. The 137 attached units that would encircle the mobile home park are located within Compatibility Zone C. Although the southeast portion of the project is located primarily within Compatibility Zone D, an approximately 2.93 acre portion is located within Compatibility Zone C. The 2.93 acre portion includes approximately 8 single-family residential lots located entirely or mostly within Compatibility Zone C. This would result in a total of 145 units within

Compatibility Zone C for a density of 3.95 dwelling units per acre. This meets the PSIALUCP density criterion of a minimum 3.0 dwelling units per acre.

The remaining 284 dwelling units that would encircle the residential neighborhood accessed via Whitewater Club Drive are located within Compatibility Zone D. With an affected area of 83.91 net acres, the density of that portion of the tract is 3.38 dwelling units per acre, also meeting the PSIALUCP density criterion of a minimum 3.0 dwelling units per acre.

Prohibited and Discouraged Uses: The proposed Tentative Tract Map would not include any prohibited uses (children's schools, day care centers, libraries, hospitals, nursing homes, buildings with greater than 3 aboveground habitable floors, highly noise sensitive outdoor nonresidential use or hazards to flight) in Compatibility Zone C or any prohibited uses (highly noise sensitive outdoor nonresidential use or hazards to flight) or discouraged uses (children's schools, hospitals, or nursing homes) in Compatibility Zone D.

Noise: The site is located within an area subject to average aircraft noise levels below 60 CNEL. Therefore, no special measures to mitigate aircraft-generated noise are required.

Open Area: Compatibility Zones C and D require a minimum of 20% and 10% open area, respectively. As previously noted, approximately 36.65 acres of the project are located within Compatibility Zone C and approximately 119.53 acres of the project are located within Compatibility Zone D. Based on these areas, 7.33 acres of open area are required within Compatibility Zone C and 11.953 acres of open area are required within Compatibility Zone D, which total 19.28 acres of open area together.

The Tentative Tract Map would provide for approximately 35.62 acres of open area within the public open space within the Whitewater wash area (consisting of the 24.93 acre remainder lot and 10.69 acre lot W), which would exceed the total minimum open area required by 16.34 acres. This open space area would remain as open area through an existing easement with Coachella Valley Water District for drainage purposes, which would generally prohibit structures and trees within the easement area. Although the project would not provide the minimum required open area required within Zone C, the Commission has in the past found projects consistent when the total open area provided exceeds the total required for each Zone together even if the open area is located in the more outer zone (ZAP1024FV08).

Part 77: The closest portion of the project is located approximately 4,334 feet northerly of Runway 13R-31L. Based on this distance and the approximate Runway 13R elevation of 474.4 feet above mean sea level (AMSL), any structure exceeding 517.74 feet AMSL would require FAA Obstruction Evaluation review. The project proposes only single-story residences with a maximum height of 24 feet in the northwestern portion and a maximum height of 18 feet in the southeastern portion. Lot 169 would be approximately the closest lot to the runway and has a pad elevation of 503.30 feet AMSL. At this pad elevation, a maximum height of 18 feet would result in a total elevation of 521.30 feet

AMSL, which would require FAA review. The site generally slopes upward from south to north as the distance from the runway increases, so additional lots may also require FAA review. At a minimum, the project has been requested to submit for FAA Obstruction Evaluation review for Lots 169, 168, 167, 166, 165, 129, 126, 125, 121, 117, 13, and 17. At the time of writing of this staff report, proof of a submittal has been provided to staff and is in a "Work in Progress" status. Aeronautical Study Numbers (ASNS) 2015-AWP-3285-OE through 2015-AWP-3291 and 2015-AWP-3293-OE through 2015-AWP-3297 have been assigned for the project. At the time of writing of this staff report, no determination has been issued by FAA.

**CONDITIONS:**

1. Any outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Lights must be downward facing.
2. The following uses shall be prohibited:
  - a. Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - b. Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - c. Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area, including landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, incinerators, fly ash disposal, and wastewater management facilities.
  - d. Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached notice shall be provided to all potential purchasers and tenants, and shall be recorded as a deed notice.
4. Any ground-level or aboveground water retention or detention basin or facilities shall be designed so as to provide for a detention period for the design storm that

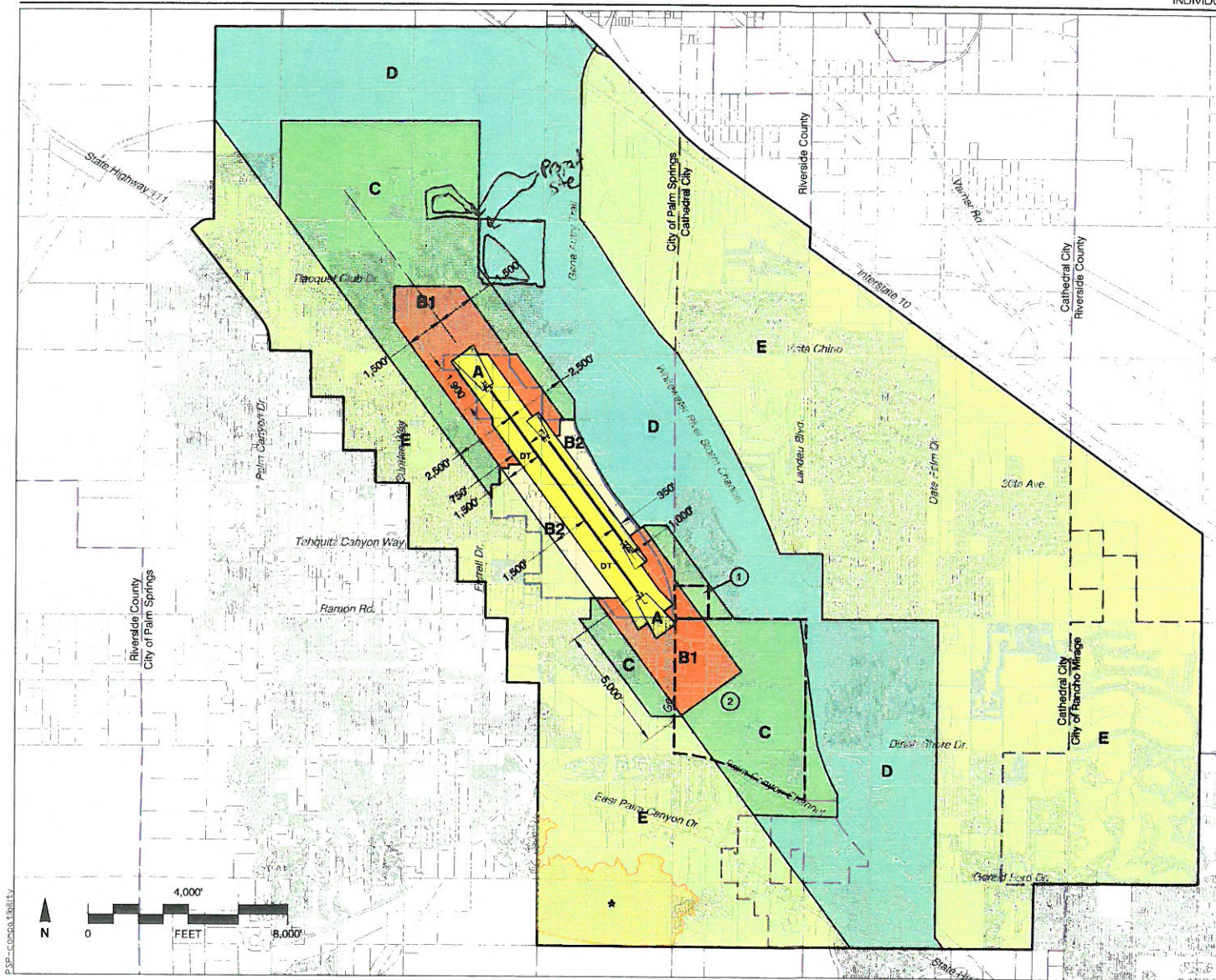
does not exceed 48 hours and to remain totally dry between rainfalls. Vegetation in and around such facilities that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature.

In the event that the requirements of this condition cannot be met, the permittee shall work with Palm Springs International Airport management and a qualified bird strike/wildlife hazard management consultant to prepare a Wildlife Hazard Management Plan that is acceptable to both the airport operator and the United States Department of Agriculture Wildlife Services agency.

5. Prior to approval of the Tentative Tract Map by the City of Palm Springs, the applicant shall have received a determination of "Not a Hazard to Air Navigation" from the Federal Aviation Administration (FAA) Obstruction Evaluation Service for the following twelve (12) lots: 13, 17, 117, 121, 125, 126, 129, 165, 166, 167, 168, and 169.
6. Prior to issuance of building permits for any lot within Tentative Tract Map No. 36691, the applicant shall provide evidence that either: (a) the elevation of the structure at its top point in feet above mean sea level would not exceed the elevation of the runway at Palm Springs International Airport at its northwesterly terminus by more than one foot for every 100 feet of distance between the structure and said southerly terminus of the runway; (b) the Federal Aviation Administration has issued a Determination of No Hazard to Air Navigation for that lot allowing for a top point elevation that equals or exceeds the proposed top point elevation; or (c) the Federal Aviation Administration Obstruction Evaluation Service has issued a statement that review of the proposal for the specific lot is not required due to its previous review and determination for the twelve (12) lots specified above.
7. During initial sales of properties within the proposed subdivision, pursuant to Palm Springs International Airport Land Use Compatibility Plan Policy 2.5(a), large airport-related informational signs clearly depicting the proximity of the property to the airport and aircraft traffic patterns shall be installed in conspicuous locations and maintained by the developer.
8. The developer shall provide to prospective buyers and/or renters an informational brochure depicting the locations of aircraft flight patterns and describing the frequency of overflights, the typical altitudes of the aircraft, and the range of noise levels that can be expected from individual aircraft overflights.

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



**Legend**

**Compatibility Zones**

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E
- Height Review Overlay Zone

**Boundary Lines**

- Airport Property Line
- City Limits

**Notes**

All dimensions measured from runway ends and centerlines.

DT = Displaced Threshold

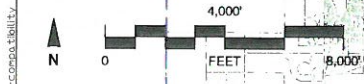
See Chapter 2, Table 2A for compatibility criteria associated with this map.

See Policy PS.2.1.

Riverside County  
 Airport Land Use Commission  
**Riverside County  
 Airport Land Use Compatibility Plan  
 Policy Document**  
 (Adopted March 2005)

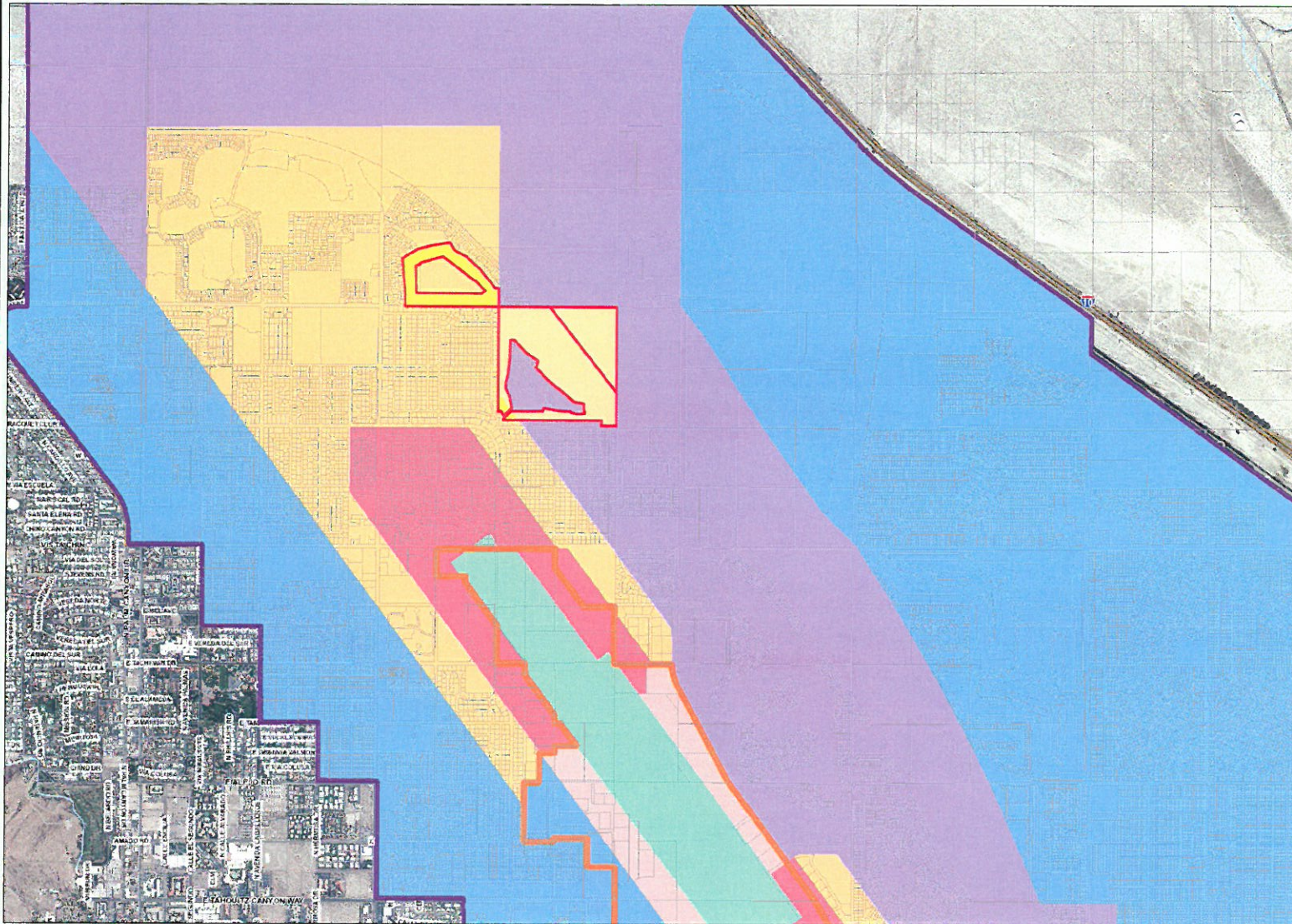
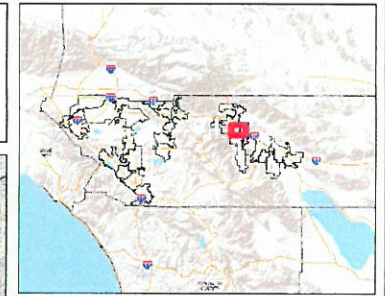
Map PS-1

**Compatibility Map**  
 Palm Springs International Airport



PSP-compatibility

# My Map



## Legend

- Airports
- AIA
- Airport Compatibility**
- <all other values>
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E
- Runways
- City Boundaries
- highways**
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- majorroads
- counties
- cities
- hydrographylines**
- waterbodies**
- Lakes
- Rivers



0 4,169 8,338 Feet



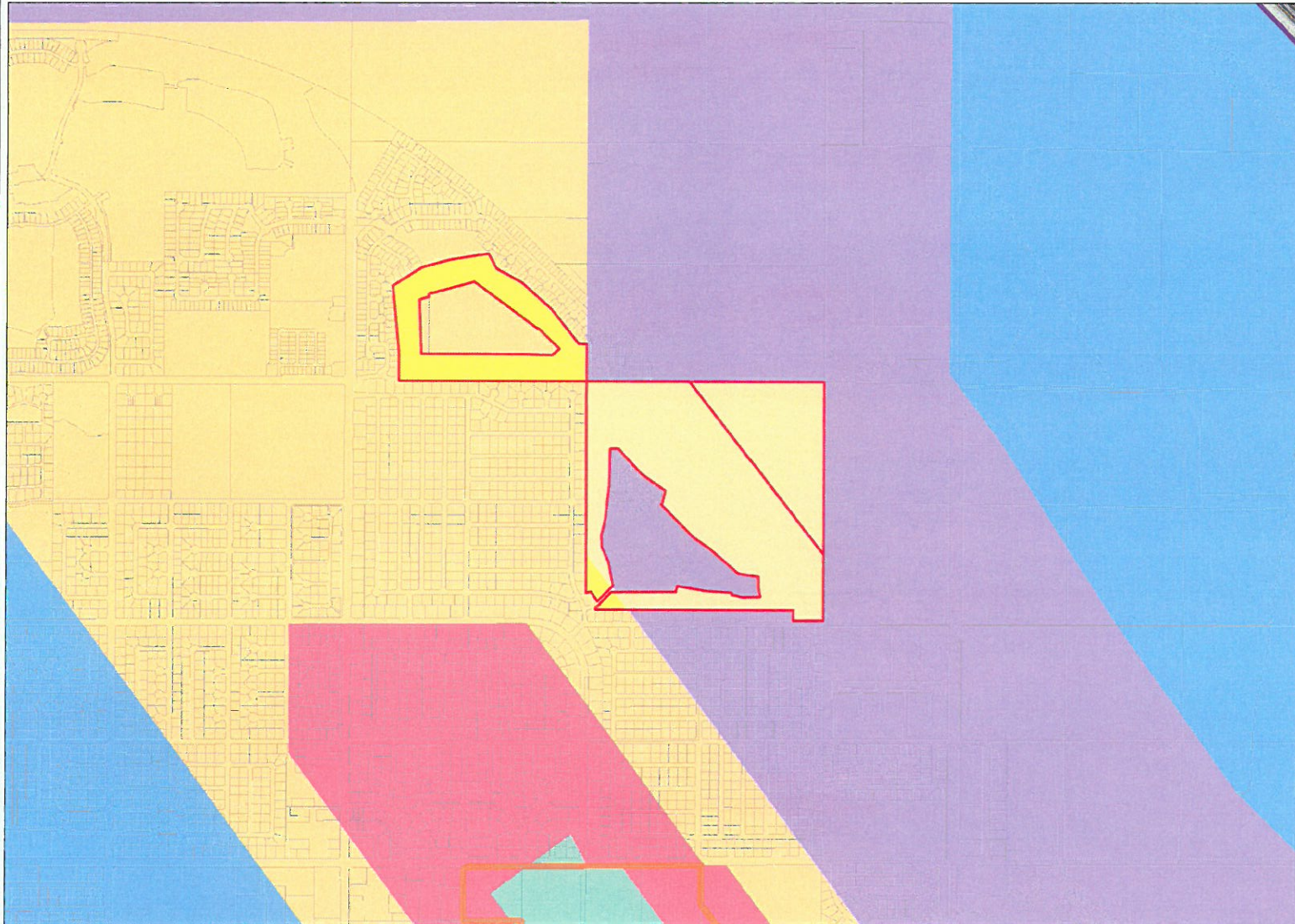
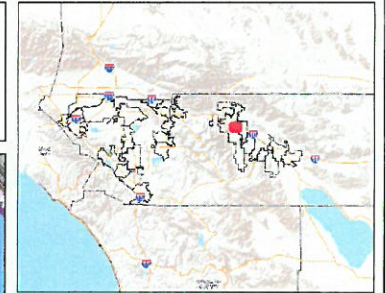
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## Notes

# My Map



## Legend

- Airports
- AIA
- Airport Compatibility**
- <all other values>
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E
- Runways
- City Boundaries
- roadsanno**
- highways**
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads**
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities



0 2,084 4,169 Feet



**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

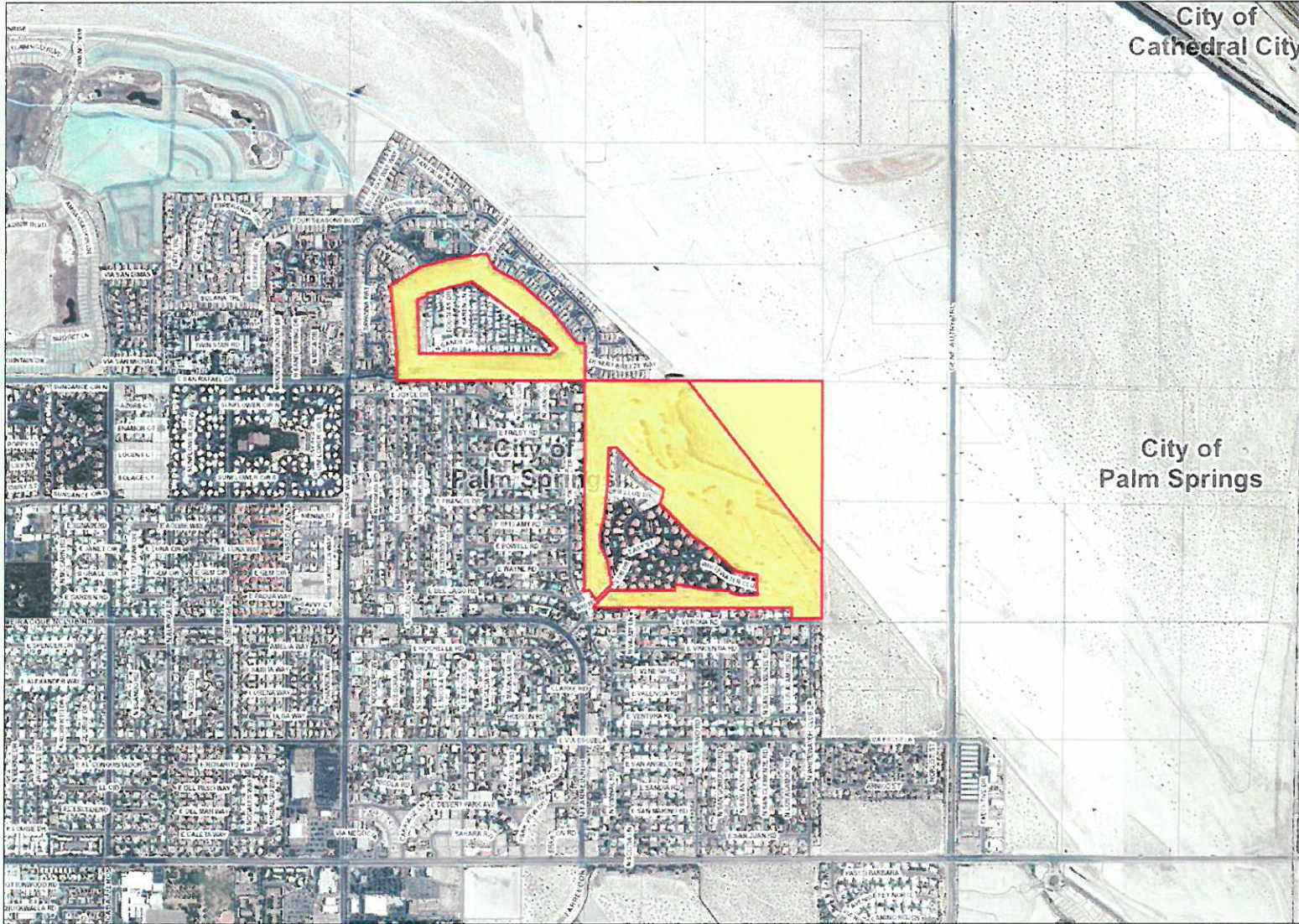
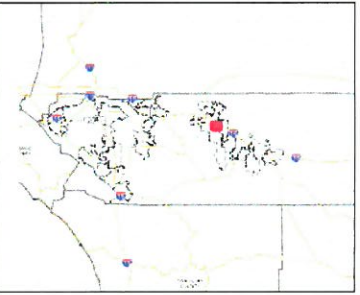
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## Notes

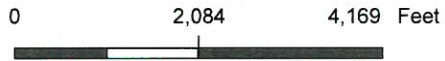


# My Map



### Legend

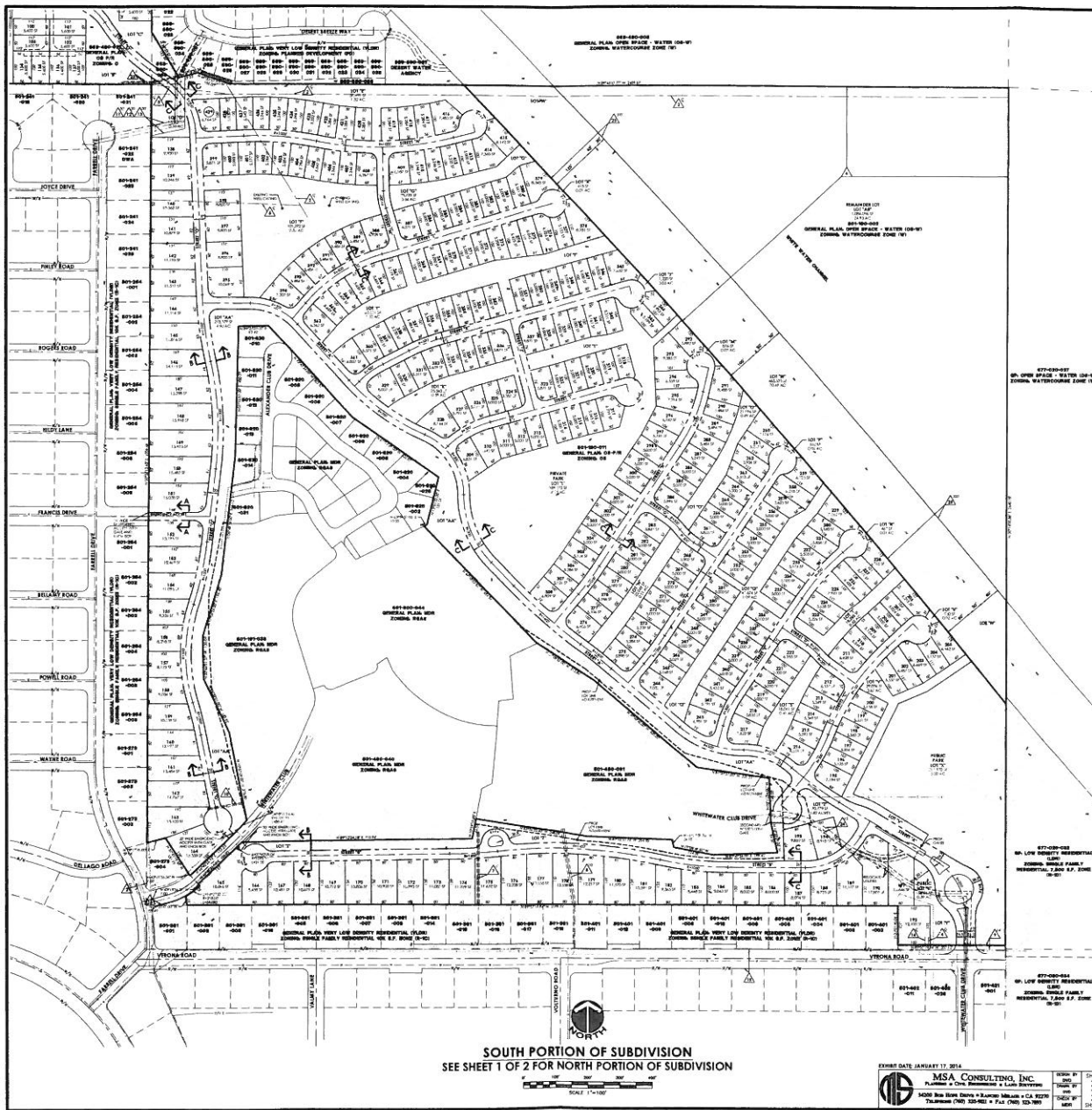
- City Boundaries
- roads
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities
- hydrography
- lines
- waterbodies
- Lakes
- Rivers



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### Notes





**SOUTH PORTION OF SUBDIVISION**  
 SEE SHEET 1 OF 2 FOR NORTH PORTION OF SUBDIVISION

SCALE 1"=100'

EXHIBIT DATE JANUARY 17, 2014

**MSA CONSULTING, INC.**  
 PLANNING & CIVIL ENGINEERING & LAND DEVELOPMENT

14000 Via Verde Drive • San Jose, CA 95131  
 Telephone (408) 252-9200 • Fax (408) 252-7891

PROJECT NO.	14000
SHEET NO.	2
TOTAL SHEETS	2
DATE	01/17/14

14000 VIA VERDE DRIVE, SAN JOSE, CA 95131  
 TEL: (408) 252-9200 FAX: (408) 252-7891





# Serena Park

## Airport Land Use Commission

<i>Lot No.</i>	<i>Pad Elevation</i>	<i>P.D.D. Building Height (1 Story)</i>	<i>Building Height Elevation</i>	<i>F.A.A. Threshold for Review</i>	<i>Distance from End of Runway</i>	<i>Difference to Threshold</i>
<b>Runway Elevation =</b>				<b>474.4</b>		
1	540.2	24 ft.	564.2 ft.	550.4 ft.	7,600 ft.	13.80 ft.
2	540.2	24 ft.	564.2 ft.	551.5 ft.	7,709 ft.	12.71 ft.
3	544.1	24 ft.	568.1 ft.	552.6 ft.	7,816 ft.	15.54 ft.
4	544.1	24 ft.	568.1 ft.	553.7 ft.	7,925 ft.	14.45 ft.
5	544.9	24 ft.	568.9 ft.	554.7 ft.	8,031 ft.	14.19 ft.
6	544.9	24 ft.	568.9 ft.	555.8 ft.	8,141 ft.	13.09 ft.
7	545.1	24 ft.	569.1 ft.	555.7 ft.	8,128 ft.	13.42 ft.
8	545.1	24 ft.	569.1 ft.	554.6 ft.	8,018 ft.	14.52 ft.
9	544.4	24 ft.	568.4 ft.	553.5 ft.	7,912 ft.	14.88 ft.
10	544.4	24 ft.	568.4 ft.	552.4 ft.	7,802 ft.	15.98 ft.
11	540.5	24 ft.	564.5 ft.	551.4 ft.	7,696 ft.	13.14 ft.
12	540.5	24 ft.	564.5 ft.	550.3 ft.	7,586 ft.	14.24 ft.
13	541.6	24 ft.	565.6 ft.	550.1 ft.	7,566 ft.	15.54 ft.
14	541.2	24 ft.	565.2 ft.	550.5 ft.	7,609 ft.	14.71 ft.
15	541.2	24 ft.	565.2 ft.	551.1 ft.	7,672 ft.	14.08 ft.
16	541.6	24 ft.	565.6 ft.	551.6 ft.	7,717 ft.	14.03 ft.
17	545.5	24 ft.	569.5 ft.	552.2 ft.	7,781 ft.	17.29 ft.
18	545.1	24 ft.	569.1 ft.	552.7 ft.	7,825 ft.	16.45 ft.
19	545.1	24 ft.	569.1 ft.	553.3 ft.	7,889 ft.	15.81 ft.
20	545.5	24 ft.	569.5 ft.	553.7 ft.	7,933 ft.	15.77 ft.
21	546.2	24 ft.	570.2 ft.	554.4 ft.	7,997 ft.	15.83 ft.
22	545.8	24 ft.	569.8 ft.	554.8 ft.	8,042 ft.	14.98 ft.
23	545.8	24 ft.	569.8 ft.	555.5 ft.	8,106 ft.	14.34 ft.
24	546.2	24 ft.	570.2 ft.	555.9 ft.	8,150 ft.	14.30 ft.
25	539.6	24 ft.	563.6 ft.	557.6 ft.	8,315 ft.	6.05 ft.
26	539.6	24 ft.	563.6 ft.	557.5 ft.	8,313 ft.	6.07 ft.
27	537.1	24 ft.	561.1 ft.	557.5 ft.	8,312 ft.	3.58 ft.
28	537.1	24 ft.	561.1 ft.	557.5 ft.	8,313 ft.	3.57 ft.
29	534.6	24 ft.	558.6 ft.	557.6 ft.	8,316 ft.	1.04 ft.
30	534.6	24 ft.	558.6 ft.	557.6 ft.	8,320 ft.	1.00 ft.
31	534.9	24 ft.	558.9 ft.	557.2 ft.	8,280 ft.	1.70 ft.
32	534.9	24 ft.	558.9 ft.	557.2 ft.	8,275 ft.	1.75 ft.
33	537.4	24 ft.	561.4 ft.	557.1 ft.	8,273 ft.	4.27 ft.
34	537.4	24 ft.	561.4 ft.	557.1 ft.	8,272 ft.	4.28 ft.
35	539.9	24 ft.	563.9 ft.	557.1 ft.	8,273 ft.	6.77 ft.
36	539.3	24 ft.	563.3 ft.	557.2 ft.	8,275 ft.	6.15 ft.

<i>Lot No.</i>	<i>Pad Elevation</i>	<i>P.D.D. Building Height (1 Story)</i>	<i>Building Height Elevation</i>	<i>F.A.A. Threshold for Review</i>	<i>Distance from End of Runway</i>	<i>Difference to Threshold</i>
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**Runway Elevation = 474.4**

37	541	24 ft.	565.0 ft.	556.2 ft.	8,176 ft.	8.84 ft.
38	540.7	24 ft.	564.7 ft.	556.1 ft.	8,174 ft.	8.56 ft.
39	540.7	24 ft.	564.7 ft.	556.1 ft.	8,173 ft.	8.57 ft.
40	541	24 ft.	565.0 ft.	556.1 ft.	8,173 ft.	8.87 ft.
41	538.5	24 ft.	562.5 ft.	556.1 ft.	8,172 ft.	6.38 ft.
42	538.1	24 ft.	562.1 ft.	556.1 ft.	8,172 ft.	5.98 ft.
43	538.1	24 ft.	562.1 ft.	556.1 ft.	8,173 ft.	5.97 ft.
44	538.5	24 ft.	562.5 ft.	556.1 ft.	8,174 ft.	6.36 ft.
45	536	24 ft.	560.0 ft.	556.2 ft.	8,175 ft.	3.85 ft.
46	535.7	24 ft.	559.7 ft.	556.2 ft.	8,177 ft.	3.53 ft.
47	535.7	24 ft.	559.7 ft.	556.2 ft.	8,178 ft.	3.52 ft.
48	536	24 ft.	560.0 ft.	555.9 ft.	8,149 ft.	4.11 ft.
49	532	24 ft.	556.0 ft.	556.0 ft.	8,156 ft.	0.04 ft.
50	532	24 ft.	556.0 ft.	555.0 ft.	8,064 ft.	0.96 ft.
51	529.6	24 ft.	553.6 ft.	554.3 ft.	7,991 ft.	-0.71 ft.
52	529.6	24 ft.	553.6 ft.	553.4 ft.	7,898 ft.	0.22 ft.
53	528.6	24 ft.	552.6 ft.	552.7 ft.	7,829 ft.	-0.09 ft.
54	528.6	24 ft.	552.6 ft.	551.8 ft.	7,740 ft.	0.80 ft.
55	525.3	24 ft.	549.3 ft.	551.0 ft.	7,662 ft.	-1.72 ft.
56	525.7	24 ft.	549.7 ft.	549.8 ft.	7,539 ft.	-0.09 ft.
57	523.9	24 ft.	547.9 ft.	549.1 ft.	7,469 ft.	-1.19 ft.
58	523.9	24 ft.	547.9 ft.	548.0 ft.	7,364 ft.	-0.14 ft.
59	525.8	24 ft.	549.8 ft.	550.8 ft.	7,638 ft.	-0.98 ft.
60	528.9	24 ft.	552.9 ft.	551.5 ft.	7,708 ft.	1.42 ft.
61	528.9	24 ft.	552.9 ft.	552.4 ft.	7,804 ft.	0.46 ft.
62	529.9	24 ft.	553.9 ft.	553.1 ft.	7,870 ft.	0.80 ft.
63	529.9	24 ft.	553.9 ft.	554.1 ft.	7,967 ft.	-0.17 ft.
64	532.3	24 ft.	556.3 ft.	554.7 ft.	8,034 ft.	1.56 ft.
65	532.3	24 ft.	556.3 ft.	555.7 ft.	8,133 ft.	0.57 ft.
66	533.4	24 ft.	557.4 ft.	555.2 ft.	8,080 ft.	2.20 ft.
67	533.1	24 ft.	557.1 ft.	554.8 ft.	8,038 ft.	2.32 ft.
68	533.1	24 ft.	557.1 ft.	554.4 ft.	7,996 ft.	2.74 ft.
69	533.4	24 ft.	557.4 ft.	553.9 ft.	7,954 ft.	3.46 ft.
70	531	24 ft.	555.0 ft.	553.5 ft.	7,912 ft.	1.48 ft.
71	530.7	24 ft.	554.7 ft.	553.1 ft.	7,871 ft.	1.59 ft.
72	530.7	24 ft.	554.7 ft.	552.7 ft.	7,830 ft.	2.00 ft.
73	531	24 ft.	555.0 ft.	552.3 ft.	7,789 ft.	2.71 ft.
74	530	24 ft.	554.0 ft.	551.9 ft.	7,748 ft.	2.12 ft.
75	529.6	24 ft.	553.6 ft.	551.5 ft.	7,707 ft.	2.13 ft.

<i>Lot No.</i>	<i>Pad Elevation</i>	<i>P.D.D. Building Height (1 Story)</i>	<i>Building Height Elevation</i>	<i>F.A.A. Threshold for Review</i>	<i>Distance from End of Runway</i>	<i>Difference to Threshold</i>
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**Runway Elevation = 474.4**

76	529.6	24 ft.	553.6 ft.	551.1 ft.	7,667 ft.	2.53 ft.
77	530	24 ft.	554.0 ft.	550.7 ft.	7,626 ft.	3.34 ft.
78	527	24 ft.	551.0 ft.	550.3 ft.	7,585 ft.	0.75 ft.
79	526.6	24 ft.	550.6 ft.	549.9 ft.	7,546 ft.	0.74 ft.
80	526.4	24 ft.	550.4 ft.	549.5 ft.	7,509 ft.	0.91 ft.
81	526.8	24 ft.	550.8 ft.	549.1 ft.	7,465 ft.	1.75 ft.
82	525	24 ft.	549.0 ft.	548.6 ft.	7,420 ft.	0.40 ft.
83	524.6	24 ft.	548.6 ft.	548.2 ft.	7,376 ft.	0.44 ft.
84	524.6	24 ft.	548.6 ft.	547.7 ft.	7,332 ft.	0.88 ft.
85	525	24 ft.	549.0 ft.	547.3 ft.	7,288 ft.	1.72 ft.
86	520	24 ft.	544.0 ft.	546.8 ft.	7,236 ft.	-2.76 ft.
87	519.6	24 ft.	543.6 ft.	546.3 ft.	7,190 ft.	-2.70 ft.
88	519.6	24 ft.	543.6 ft.	545.6 ft.	7,124 ft.	-2.04 ft.
89	520	24 ft.	544.0 ft.	545.2 ft.	7,079 ft.	-1.19 ft.
90	531.3	24 ft.	555.3 ft.	547.5 ft.	7,308 ft.	7.82 ft.
91	531.3	24 ft.	555.3 ft.	547.1 ft.	7,273 ft.	8.17 ft.
92	530	24 ft.	554.0 ft.	546.9 ft.	7,246 ft.	7.14 ft.
93	530	24 ft.	554.0 ft.	546.6 ft.	7,215 ft.	7.45 ft.
94	527.7	24 ft.	551.7 ft.	546.3 ft.	7,191 ft.	5.39 ft.
95	527.7	24 ft.	551.7 ft.	546.0 ft.	7,163 ft.	5.67 ft.
96	525.4	24 ft.	549.4 ft.	545.8 ft.	7,142 ft.	3.58 ft.
97	525.4	24 ft.	549.4 ft.	545.6 ft.	7,119 ft.	3.81 ft.
98	523.1	24 ft.	547.1 ft.	545.4 ft.	7,101 ft.	1.69 ft.
99	523.1	24 ft.	547.1 ft.	545.2 ft.	7,081 ft.	1.89 ft.
100	519	24 ft.	543.0 ft.	545.0 ft.	7,060 ft.	-2.00 ft.
101	519	24 ft.	543.0 ft.	544.9 ft.	7,045 ft.	-1.85 ft.
102	519.3	24 ft.	543.3 ft.	544.4 ft.	7,004 ft.	-1.14 ft.
103	519.3	24 ft.	543.3 ft.	544.6 ft.	7,022 ft.	-1.32 ft.
104	523.4	24 ft.	547.4 ft.	544.8 ft.	7,040 ft.	2.60 ft.
105	523.4	24 ft.	547.4 ft.	545.0 ft.	7,062 ft.	2.38 ft.
106	525.7	24 ft.	549.7 ft.	545.2 ft.	7,078 ft.	4.52 ft.
107	525.7	24 ft.	549.7 ft.	545.4 ft.	7,104 ft.	4.26 ft.
108	528	24 ft.	552.0 ft.	545.6 ft.	7,123 ft.	6.37 ft.
109	528	24 ft.	552.0 ft.	545.9 ft.	7,153 ft.	6.07 ft.
110	530.3	24 ft.	554.3 ft.	546.2 ft.	7,175 ft.	8.15 ft.
111	530.3	24 ft.	554.3 ft.	546.5 ft.	7,209 ft.	7.81 ft.
112	531.6	24 ft.	555.6 ft.	546.7 ft.	7,233 ft.	8.87 ft.
113	531.6	24 ft.	555.6 ft.	547.1 ft.	7,271 ft.	8.49 ft.
114	532.7	24 ft.	556.7 ft.	546.2 ft.	7,180 ft.	10.50 ft.



<i>Lot No.</i>	<i>Pad Elevation</i>	<i>P.D.D. Building Height (1 Story)</i>	<i>Building Height Elevation</i>	<i>F.A.A. Threshold for Review</i>	<i>Distance from End of Runway</i>	<i>Difference to Threshold</i>
<b>Runway Elevation =</b>				<b>474.4</b>		
115	532.3	24 ft.	556.3 ft.	546.0 ft.	7,164 ft.	10.26 ft.
116	532.3	24 ft.	556.3 ft.	545.9 ft.	7,148 ft.	10.42 ft.
117	532.7	24 ft.	556.7 ft.	545.7 ft.	7,132 ft.	10.98 ft.
118	531.4	24 ft.	555.4 ft.	545.6 ft.	7,117 ft.	9.83 ft.
119	531	24 ft.	555.0 ft.	545.4 ft.	7,102 ft.	9.58 ft.
120	531	24 ft.	555.0 ft.	545.3 ft.	7,088 ft.	9.72 ft.
121	531.4	24 ft.	555.4 ft.	545.1 ft.	7,074 ft.	10.26 ft.
122	529.1	24 ft.	553.1 ft.	545.0 ft.	7,060 ft.	8.10 ft.
123	528.7	24 ft.	552.7 ft.	544.9 ft.	7,047 ft.	7.83 ft.
124	528.7	24 ft.	552.7 ft.	544.7 ft.	7,034 ft.	7.96 ft.
125	529.1	24 ft.	553.1 ft.	544.6 ft.	7,022 ft.	8.48 ft.
126	526.8	24 ft.	550.8 ft.	544.5 ft.	7,010 ft.	6.30 ft.
127	525.4	24 ft.	549.4 ft.	544.4 ft.	6,998 ft.	5.02 ft.
128	525.4	24 ft.	549.4 ft.	544.3 ft.	6,987 ft.	5.13 ft.
129	525.8	24 ft.	549.8 ft.	544.2 ft.	6,977 ft.	5.63 ft.
130	524.4	24 ft.	548.4 ft.	544.1 ft.	6,967 ft.	4.33 ft.
131	524.1	24 ft.	548.1 ft.	544.0 ft.	6,957 ft.	4.13 ft.
132	524.1	24 ft.	548.1 ft.	543.9 ft.	6,948 ft.	4.22 ft.
133	524.5	24 ft.	548.5 ft.	543.8 ft.	6,939 ft.	4.71 ft.
134	520.4	24 ft.	544.4 ft.	543.7 ft.	6,925 ft.	0.75 ft.
135	520	24 ft.	544.0 ft.	543.6 ft.	6,917 ft.	0.43 ft.
136	520	24 ft.	544.0 ft.	543.5 ft.	6,910 ft.	0.50 ft.
137	520.4	24 ft.	544.4 ft.	543.4 ft.	6,903 ft.	0.97 ft.
138	514.5	18 ft.	532.5 ft.	540.7 ft.	6,626 ft.	-8.16 ft.
139	513.4	18 ft.	531.4 ft.	539.8 ft.	6,544 ft.	-8.44 ft.
140	513.1	18 ft.	531.1 ft.	539.0 ft.	6,463 ft.	-7.93 ft.
141	513.6	18 ft.	531.6 ft.	538.2 ft.	6,381 ft.	-6.61 ft.
142	514.1	18 ft.	532.1 ft.	537.4 ft.	6,299 ft.	-5.29 ft.
143	514.6	18 ft.	532.6 ft.	536.6 ft.	6,217 ft.	-3.97 ft.
144	514.9	18 ft.	532.9 ft.	535.8 ft.	6,135 ft.	-2.85 ft.
145	514.5	18 ft.	532.5 ft.	534.9 ft.	6,053 ft.	-2.43 ft.
146	514.1	18 ft.	532.1 ft.	534.1 ft.	5,970 ft.	-2.00 ft.
147	513.6	18 ft.	531.6 ft.	533.3 ft.	5,888 ft.	-1.68 ft.
148	513.2	18 ft.	531.2 ft.	532.5 ft.	5,806 ft.	-1.26 ft.
149	512.8	18 ft.	530.8 ft.	531.7 ft.	5,725 ft.	-0.85 ft.
150	512.4	18 ft.	530.4 ft.	530.8 ft.	5,643 ft.	-0.43 ft.
151	511.9	18 ft.	529.9 ft.	530.0 ft.	5,559 ft.	-0.09 ft.
152	511.2	18 ft.	529.2 ft.	528.9 ft.	5,449 ft.	0.31 ft.
153	510.9	18 ft.	528.9 ft.	528.1 ft.	5,370 ft.	0.80 ft.

<i>Lot No.</i>	<i>Pad Elevation</i>	<i>P.D.D. Building Height (1 Story)</i>	<i>Building Height Elevation</i>	<i>F.A.A. Threshold for Review</i>	<i>Distance from End of Runway</i>	<i>Difference to Threshold</i>
<b>Runway Elevation = 474.4</b>						
154	510.5	18 ft.	528.5 ft.	527.3 ft.	5,292 ft.	1.18 ft.
155	510.1	18 ft.	528.1 ft.	526.5 ft.	5,213 ft.	1.57 ft.
156	509.7	18 ft.	527.7 ft.	525.7 ft.	5,134 ft.	1.96 ft.
157	509.3	18 ft.	527.3 ft.	524.9 ft.	5,054 ft.	2.36 ft.
158	508.9	18 ft.	526.9 ft.	524.1 ft.	4,974 ft.	2.76 ft.
159	508.5	18 ft.	526.5 ft.	523.3 ft.	4,893 ft.	3.17 ft.
160	508	18 ft.	526.0 ft.	522.5 ft.	4,812 ft.	3.48 ft.
161	507.6	18 ft.	525.6 ft.	521.7 ft.	4,732 ft.	3.88 ft.
162	507.2	18 ft.	525.2 ft.	520.9 ft.	4,652 ft.	4.28 ft.
163	506.8	18 ft.	524.8 ft.	520.2 ft.	4,575 ft.	4.65 ft.
164	506.5	18 ft.	524.5 ft.	519.2 ft.	4,483 ft.	5.27 ft.
165	505.4	18 ft.	523.4 ft.	517.9 ft.	4,346 ft.	5.54 ft.
166	505.1	18 ft.	523.1 ft.	517.8 ft.	4,341 ft.	5.29 ft.
167	504.7	18 ft.	522.7 ft.	517.8 ft.	4,337 ft.	4.93 ft.
168	504	18 ft.	522.0 ft.	517.8 ft.	4,335 ft.	4.25 ft.
169	503.3	18 ft.	521.3 ft.	517.7 ft.	4,334 ft.	3.56 ft.
170	502.6	18 ft.	520.6 ft.	517.8 ft.	4,335 ft.	2.85 ft.
171	501.9	18 ft.	519.9 ft.	517.8 ft.	4,337 ft.	2.13 ft.
172	501.3	18 ft.	519.3 ft.	517.8 ft.	4,341 ft.	1.49 ft.
173	500.6	18 ft.	518.6 ft.	517.9 ft.	4,346 ft.	0.74 ft.
174	499.9	18 ft.	517.9 ft.	517.9 ft.	4,353 ft.	-0.03 ft.
175	499.2	18 ft.	517.2 ft.	518.0 ft.	4,361 ft.	-0.81 ft.
176	498.5	18 ft.	516.5 ft.	518.1 ft.	4,371 ft.	-1.61 ft.
177	497.8	18 ft.	515.8 ft.	518.2 ft.	4,382 ft.	-2.42 ft.
178	497.1	18 ft.	515.1 ft.	518.3 ft.	4,394 ft.	-3.24 ft.
179	496.4	18 ft.	514.4 ft.	518.5 ft.	4,408 ft.	-4.08 ft.
180	495.8	18 ft.	513.8 ft.	518.6 ft.	4,423 ft.	-4.83 ft.
181	495.1	18 ft.	513.1 ft.	518.8 ft.	4,440 ft.	-5.70 ft.
182	494.4	18 ft.	512.4 ft.	519.0 ft.	4,458 ft.	-6.58 ft.
183	493.7	18 ft.	511.7 ft.	519.2 ft.	4,477 ft.	-7.47 ft.
184	493	18 ft.	511.0 ft.	519.4 ft.	4,498 ft.	-8.38 ft.
185	492.3	18 ft.	510.3 ft.	519.6 ft.	4,520 ft.	-9.30 ft.
186	491.6	18 ft.	509.6 ft.	519.8 ft.	4,544 ft.	-10.24 ft.
187	490.9	18 ft.	508.9 ft.	520.1 ft.	4,568 ft.	-11.18 ft.
188	490.3	18 ft.	508.3 ft.	520.3 ft.	4,594 ft.	-12.04 ft.
189	489.6	18 ft.	507.6 ft.	520.6 ft.	4,621 ft.	-13.01 ft.
190	488.9	18 ft.	506.9 ft.	520.9 ft.	4,650 ft.	-14.00 ft.
191	488.1	18 ft.	506.1 ft.	521.2 ft.	4,679 ft.	-15.09 ft.
192	486.2	18 ft.	504.2 ft.	520.2 ft.	4,583 ft.	-16.03 ft.

<i>Lot No.</i>	<i>Pad Elevation</i>	<i>P.D.D. Building Height (1 Story)</i>	<i>Building Height Elevation</i>	<i>F.A.A. Threshold for Review</i>	<i>Distance from End of Runway</i>	<i>Difference to Threshold</i>
<b>Runway Elevation =</b>				<b>474.4</b>		
193	490.9	18 ft.	508.9 ft.	521.3 ft.	4,693 ft.	-12.43 ft.
194	489.9	18 ft.	507.9 ft.	521.8 ft.	4,743 ft.	-13.93 ft.
195	490.1	24 ft.	514.1 ft.	523.8 ft.	4,937 ft.	-9.67 ft.
196	490.3	24 ft.	514.3 ft.	524.4 ft.	5,003 ft.	-10.13 ft.
197	490.6	24 ft.	514.6 ft.	524.9 ft.	5,052 ft.	-10.32 ft.
198	491	24 ft.	515.0 ft.	525.4 ft.	5,102 ft.	-10.42 ft.
199	491.3	24 ft.	515.3 ft.	525.9 ft.	5,152 ft.	-10.62 ft.
200	491.6	24 ft.	515.6 ft.	526.4 ft.	5,201 ft.	-10.81 ft.
201	492.4	24 ft.	516.4 ft.	527.3 ft.	5,288 ft.	-10.88 ft.
202	492.7	24 ft.	516.7 ft.	527.6 ft.	5,322 ft.	-10.92 ft.
203	493	24 ft.	517.0 ft.	528.0 ft.	5,359 ft.	-10.99 ft.
204	493.2	24 ft.	517.2 ft.	528.4 ft.	5,400 ft.	-11.20 ft.
205	493.5	24 ft.	517.5 ft.	528.8 ft.	5,444 ft.	-11.34 ft.
206	493.5	24 ft.	517.5 ft.	529.9 ft.	5,546 ft.	-12.36 ft.
207	493.2	24 ft.	517.2 ft.	529.4 ft.	5,503 ft.	-12.23 ft.
208	493	24 ft.	517.0 ft.	529.0 ft.	5,460 ft.	-12.00 ft.
209	492.7	24 ft.	516.7 ft.	528.6 ft.	5,415 ft.	-11.85 ft.
210	492.4	24 ft.	516.4 ft.	528.1 ft.	5,367 ft.	-11.67 ft.
211	492.1	24 ft.	516.1 ft.	527.5 ft.	5,312 ft.	-11.42 ft.
212	491.5	24 ft.	515.5 ft.	526.5 ft.	5,207 ft.	-10.97 ft.
213	491.1	24 ft.	515.1 ft.	526.0 ft.	5,155 ft.	-10.85 ft.
214	490.9	24 ft.	514.9 ft.	525.4 ft.	5,102 ft.	-10.52 ft.
215	490.6	24 ft.	514.6 ft.	524.9 ft.	5,046 ft.	-10.26 ft.
216	490.2	24 ft.	514.2 ft.	524.3 ft.	4,994 ft.	-10.14 ft.
217	492.1	24 ft.	516.1 ft.	524.4 ft.	4,995 ft.	-8.25 ft.
218	492.5	24 ft.	516.5 ft.	525.1 ft.	5,067 ft.	-8.57 ft.
219	492.8	24 ft.	516.8 ft.	525.6 ft.	5,123 ft.	-8.83 ft.
220	493.1	24 ft.	517.1 ft.	526.1 ft.	5,170 ft.	-9.00 ft.
221	493.4	24 ft.	517.4 ft.	526.6 ft.	5,218 ft.	-9.18 ft.
222	493.7	24 ft.	517.7 ft.	527.1 ft.	5,265 ft.	-9.35 ft.
223	494.5	24 ft.	518.5 ft.	528.3 ft.	5,394 ft.	-9.84 ft.
224	494.8	24 ft.	518.8 ft.	528.8 ft.	5,440 ft.	-10.00 ft.
225	495.3	24 ft.	519.3 ft.	529.3 ft.	5,486 ft.	-9.96 ft.
226	495.6	24 ft.	519.6 ft.	529.8 ft.	5,542 ft.	-10.22 ft.
227	495.9	24 ft.	519.9 ft.	530.3 ft.	5,585 ft.	-10.35 ft.
228	496.2	24 ft.	520.2 ft.	530.6 ft.	5,623 ft.	-10.43 ft.
229	496.2	24 ft.	520.2 ft.	531.6 ft.	5,718 ft.	-11.38 ft.
230	495.9	24 ft.	519.9 ft.	531.1 ft.	5,670 ft.	-11.20 ft.
231	495.7	24 ft.	519.7 ft.	530.7 ft.	5,625 ft.	-10.95 ft.

<i>Lot No.</i>	<i>Pad Elevation</i>	<i>P.D.D. Building Height (1 Story)</i>	<i>Building Height Elevation</i>	<i>F.A.A. Threshold for Review</i>	<i>Distance from End of Runway</i>	<i>Difference to Threshold</i>
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**Runway Elevation = 474.4**

232	495.4	24 ft.	519.4 ft.	530.2 ft.	5,579 ft.	-10.79 ft.
233	495.1	24 ft.	519.1 ft.	529.7 ft.	5,532 ft.	-10.62 ft.
234	494.8	24 ft.	518.8 ft.	529.3 ft.	5,485 ft.	-10.45 ft.
235	494.5	24 ft.	518.5 ft.	528.8 ft.	5,438 ft.	-10.28 ft.
236	494	24 ft.	518.0 ft.	528.0 ft.	5,360 ft.	-10.00 ft.
237	493.7	24 ft.	517.7 ft.	527.5 ft.	5,313 ft.	-9.83 ft.
238	493.4	24 ft.	517.4 ft.	527.1 ft.	5,266 ft.	-9.66 ft.
239	493.1	24 ft.	517.1 ft.	526.6 ft.	5,219 ft.	-9.49 ft.
240	492.8	24 ft.	516.8 ft.	526.1 ft.	5,172 ft.	-9.32 ft.
241	492.5	24 ft.	516.5 ft.	525.6 ft.	5,121 ft.	-9.11 ft.
242	492.2	24 ft.	516.2 ft.	525.0 ft.	5,064 ft.	-8.84 ft.
243	491.9	24 ft.	515.9 ft.	524.4 ft.	5,001 ft.	-8.51 ft.
244	493.9	24 ft.	517.9 ft.	525.1 ft.	5,074 ft.	-7.24 ft.
245	494.2	24 ft.	518.2 ft.	525.7 ft.	5,134 ft.	-7.54 ft.
246	494.5	24 ft.	518.5 ft.	526.2 ft.	5,180 ft.	-7.70 ft.
247	494.8	24 ft.	518.8 ft.	526.7 ft.	5,226 ft.	-7.86 ft.
248	495.1	24 ft.	519.1 ft.	527.1 ft.	5,272 ft.	-8.02 ft.
249	495.4	24 ft.	519.4 ft.	527.6 ft.	5,319 ft.	-8.19 ft.
250	495.7	24 ft.	519.7 ft.	528.1 ft.	5,365 ft.	-8.35 ft.
251	496	24 ft.	520.0 ft.	528.5 ft.	5,412 ft.	-8.52 ft.
252	496.4	24 ft.	520.4 ft.	529.2 ft.	5,478 ft.	-8.78 ft.
253	496.7	24 ft.	520.7 ft.	529.7 ft.	5,525 ft.	-8.95 ft.
254	497	24 ft.	521.0 ft.	530.1 ft.	5,572 ft.	-9.12 ft.
255	497.3	24 ft.	521.3 ft.	530.6 ft.	5,619 ft.	-9.29 ft.
256	497.6	24 ft.	521.6 ft.	531.1 ft.	5,666 ft.	-9.46 ft.
257	497.9	24 ft.	521.9 ft.	531.5 ft.	5,713 ft.	-9.63 ft.
258	498.2	24 ft.	522.2 ft.	532.0 ft.	5,755 ft.	-9.75 ft.
259	498.5	24 ft.	522.5 ft.	532.4 ft.	5,801 ft.	-9.91 ft.
260	498.8	24 ft.	522.8 ft.	533.5 ft.	5,910 ft.	-10.70 ft.
261	498.5	24 ft.	522.5 ft.	533.0 ft.	5,860 ft.	-10.50 ft.
262	498.2	24 ft.	522.2 ft.	532.5 ft.	5,810 ft.	-10.30 ft.
263	497.9	24 ft.	521.9 ft.	532.0 ft.	5,761 ft.	-10.11 ft.
264	497.6	24 ft.	521.6 ft.	531.6 ft.	5,715 ft.	-9.95 ft.
265	497.3	24 ft.	521.3 ft.	531.1 ft.	5,668 ft.	-9.78 ft.
266	497	24 ft.	521.0 ft.	530.6 ft.	5,622 ft.	-9.62 ft.
267	496.7	24 ft.	520.7 ft.	530.2 ft.	5,577 ft.	-9.47 ft.
268	496.1	24 ft.	520.1 ft.	529.2 ft.	5,476 ft.	-9.06 ft.
269	495.8	24 ft.	519.8 ft.	528.7 ft.	5,430 ft.	-8.90 ft.
270	495.5	24 ft.	519.5 ft.	528.2 ft.	5,384 ft.	-8.74 ft.

<i>Lot No.</i>	<i>Pad Elevation</i>	<i>P.D.D. Building Height (1 Story)</i>	<i>Building Height Elevation</i>	<i>F.A.A. Threshold for Review</i>	<i>Distance from End of Runway</i>	<i>Difference to Threshold</i>
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**Runway Elevation = 474.4**

271	495.2	24 ft.	519.2 ft.	527.8 ft.	5,338 ft.	-8.58 ft.
272	494.9	24 ft.	518.9 ft.	527.3 ft.	5,292 ft.	-8.42 ft.
273	494.6	24 ft.	518.6 ft.	526.8 ft.	5,244 ft.	-8.24 ft.
274	494.2	24 ft.	518.2 ft.	526.4 ft.	5,198 ft.	-8.18 ft.
275	493.9	24 ft.	517.9 ft.	526.0 ft.	5,160 ft.	-8.10 ft.
276	496.9	24 ft.	520.9 ft.	526.6 ft.	5,222 ft.	-5.72 ft.
277	497.2	24 ft.	521.2 ft.	527.2 ft.	5,277 ft.	-5.97 ft.
278	497.4	24 ft.	521.4 ft.	527.6 ft.	5,321 ft.	-6.21 ft.
279	497.7	24 ft.	521.7 ft.	528.1 ft.	5,367 ft.	-6.37 ft.
280	498	24 ft.	522.0 ft.	528.5 ft.	5,414 ft.	-6.54 ft.
281	498.3	24 ft.	522.3 ft.	529.0 ft.	5,460 ft.	-6.70 ft.
282	498.6	24 ft.	522.6 ft.	529.5 ft.	5,505 ft.	-6.85 ft.
283	499	24 ft.	523.0 ft.	529.9 ft.	5,551 ft.	-6.91 ft.
284	499.6	24 ft.	523.6 ft.	530.9 ft.	5,646 ft.	-7.26 ft.
285	499.9	24 ft.	523.9 ft.	531.4 ft.	5,695 ft.	-7.45 ft.
286	500.2	24 ft.	524.2 ft.	531.8 ft.	5,741 ft.	-7.61 ft.
287	500.5	24 ft.	524.5 ft.	532.3 ft.	5,787 ft.	-7.77 ft.
288	500.8	24 ft.	524.8 ft.	532.8 ft.	5,836 ft.	-7.96 ft.
289	501.1	24 ft.	525.1 ft.	533.3 ft.	5,888 ft.	-8.18 ft.
290	501.3	24 ft.	525.3 ft.	533.8 ft.	5,942 ft.	-8.52 ft.
291	501.8	24 ft.	525.8 ft.	534.4 ft.	5,997 ft.	-8.57 ft.
292	502.4	24 ft.	526.4 ft.	535.8 ft.	6,143 ft.	-9.43 ft.
293	502.1	24 ft.	526.1 ft.	535.0 ft.	6,060 ft.	-8.90 ft.
294	501.8	24 ft.	525.8 ft.	534.5 ft.	6,010 ft.	-8.70 ft.
295	501.5	24 ft.	525.5 ft.	533.8 ft.	5,944 ft.	-8.34 ft.
296	501	24 ft.	525.0 ft.	533.2 ft.	5,883 ft.	-8.23 ft.
297	500.5	24 ft.	524.5 ft.	532.7 ft.	5,831 ft.	-8.21 ft.
298	500.1	24 ft.	524.1 ft.	532.3 ft.	5,785 ft.	-8.15 ft.
299	499.8	24 ft.	523.8 ft.	531.8 ft.	5,740 ft.	-8.00 ft.
300	499.5	24 ft.	523.5 ft.	531.3 ft.	5,694 ft.	-7.84 ft.
301	499	24 ft.	523.0 ft.	530.6 ft.	5,622 ft.	-7.62 ft.
302	498.7	24 ft.	522.7 ft.	530.2 ft.	5,576 ft.	-7.46 ft.
303	498.4	24 ft.	522.4 ft.	529.7 ft.	5,531 ft.	-7.31 ft.
304	498.1	24 ft.	522.1 ft.	529.3 ft.	5,486 ft.	-7.16 ft.
305	497.8	24 ft.	521.8 ft.	528.8 ft.	5,440 ft.	-7.00 ft.
306	497.5	24 ft.	521.5 ft.	528.3 ft.	5,394 ft.	-6.84 ft.
307	497.2	24 ft.	521.2 ft.	527.9 ft.	5,350 ft.	-6.70 ft.
308	496.9	24 ft.	520.9 ft.	527.4 ft.	5,303 ft.	-6.53 ft.
309	505.8	24 ft.	529.8 ft.	531.1 ft.	5,674 ft.	-1.34 ft.

<i>Lot No.</i>	<i>Pad Elevation</i>	<i>P.D.D. Building Height (1 Story)</i>	<i>Building Height Elevation</i>	<i>F.A.A. Threshold for Review</i>	<i>Distance from End of Runway</i>	<i>Difference to Threshold</i>
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**Runway Elevation = 474.4**

310	505.1	24 ft.	529.1 ft.	531.5 ft.	5,712 ft.	-2.42 ft.
311	504.1	24 ft.	528.1 ft.	531.8 ft.	5,737 ft.	-3.67 ft.
312	503.5	24 ft.	527.5 ft.	532.0 ft.	5,755 ft.	-4.45 ft.
313	502.8	24 ft.	526.8 ft.	532.1 ft.	5,773 ft.	-5.33 ft.
314	502.9	24 ft.	526.9 ft.	532.4 ft.	5,803 ft.	-5.53 ft.
315	503.2	24 ft.	527.2 ft.	532.6 ft.	5,822 ft.	-5.42 ft.
316	503.5	24 ft.	527.5 ft.	532.8 ft.	5,841 ft.	-5.31 ft.
317	503.9	24 ft.	527.9 ft.	533.0 ft.	5,861 ft.	-5.11 ft.
318	504.3	24 ft.	528.3 ft.	534.6 ft.	6,024 ft.	-6.34 ft.
319	504	24 ft.	528.0 ft.	534.4 ft.	6,004 ft.	-6.44 ft.
320	503.7	24 ft.	527.7 ft.	534.2 ft.	5,984 ft.	-6.54 ft.
321	503.4	24 ft.	527.4 ft.	534.1 ft.	5,965 ft.	-6.65 ft.
322	503.1	24 ft.	527.1 ft.	533.9 ft.	5,946 ft.	-6.76 ft.
323	502.8	24 ft.	526.8 ft.	533.7 ft.	5,926 ft.	-6.86 ft.
324	503.4	24 ft.	527.4 ft.	533.3 ft.	5,886 ft.	-5.86 ft.
325	504.1	24 ft.	528.1 ft.	533.1 ft.	5,867 ft.	-4.97 ft.
326	504.7	24 ft.	528.7 ft.	532.9 ft.	5,848 ft.	-4.18 ft.
327	505.3	24 ft.	529.3 ft.	532.7 ft.	5,825 ft.	-3.35 ft.
328	505.8	24 ft.	529.8 ft.	532.3 ft.	5,792 ft.	-2.52 ft.
329	508	24 ft.	532.0 ft.	533.2 ft.	5,878 ft.	-1.18 ft.
330	507.5	24 ft.	531.5 ft.	533.5 ft.	5,910 ft.	-2.00 ft.
331	507.2	24 ft.	531.2 ft.	533.8 ft.	5,936 ft.	-2.56 ft.
332	506.8	24 ft.	530.8 ft.	534.0 ft.	5,958 ft.	-3.18 ft.
333	506.4	24 ft.	530.4 ft.	534.2 ft.	5,977 ft.	-3.77 ft.
334	506.1	24 ft.	530.1 ft.	534.3 ft.	5,993 ft.	-4.23 ft.
335	505.8	24 ft.	529.8 ft.	534.5 ft.	6,009 ft.	-4.69 ft.
336	505.5	24 ft.	529.5 ft.	534.7 ft.	6,028 ft.	-5.18 ft.
337	503.8	24 ft.	527.8 ft.	535.0 ft.	6,063 ft.	-7.23 ft.
338	504.1	24 ft.	528.1 ft.	535.2 ft.	6,083 ft.	-7.13 ft.
339	504.4	24 ft.	528.4 ft.	535.4 ft.	6,102 ft.	-7.02 ft.
340	504.7	24 ft.	528.7 ft.	535.6 ft.	6,121 ft.	-6.91 ft.
341	505	24 ft.	529.0 ft.	535.8 ft.	6,137 ft.	-6.77 ft.
342	505.3	24 ft.	529.3 ft.	535.9 ft.	6,154 ft.	-6.64 ft.
343	505.7	24 ft.	529.7 ft.	536.2 ft.	6,180 ft.	-6.50 ft.
344	506	24 ft.	530.0 ft.	536.6 ft.	6,219 ft.	-6.59 ft.
345	505.8	24 ft.	529.8 ft.	537.6 ft.	6,316 ft.	-7.76 ft.
346	505.4	24 ft.	529.4 ft.	537.3 ft.	6,288 ft.	-7.88 ft.
347	505.1	24 ft.	529.1 ft.	537.1 ft.	6,268 ft.	-7.98 ft.
348	504.8	24 ft.	528.8 ft.	536.9 ft.	6,249 ft.	-8.09 ft.

<i>Lot No.</i>	<i>Pad Elevation</i>	<i>P.D.D. Building Height (1 Story)</i>	<i>Building Height Elevation</i>	<i>F.A.A. Threshold for Review</i>	<i>Distance from End of Runway</i>	<i>Difference to Threshold</i>
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**Runway Elevation = 474.4**

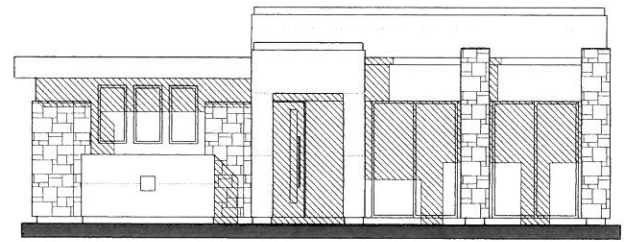
349	504.5	24 ft.	528.5 ft.	536.7 ft.	6,231 ft.	-8.21 ft.
350	504.2	24 ft.	528.2 ft.	536.5 ft.	6,213 ft.	-8.33 ft.
351	503.9	24 ft.	527.9 ft.	536.4 ft.	6,195 ft.	-8.45 ft.
352	505.4	24 ft.	529.4 ft.	536.0 ft.	6,164 ft.	-6.64 ft.
353	505.7	24 ft.	529.7 ft.	535.9 ft.	6,147 ft.	-6.17 ft.
354	506	24 ft.	530.0 ft.	535.7 ft.	6,131 ft.	-5.71 ft.
355	506.3	24 ft.	530.3 ft.	535.5 ft.	6,114 ft.	-5.24 ft.
356	506.6	24 ft.	530.6 ft.	535.4 ft.	6,096 ft.	-4.76 ft.
357	506.9	24 ft.	530.9 ft.	535.2 ft.	6,079 ft.	-4.29 ft.
358	507.2	24 ft.	531.2 ft.	535.0 ft.	6,056 ft.	-3.76 ft.
359	507.5	24 ft.	531.5 ft.	534.7 ft.	6,031 ft.	-3.21 ft.
360	507.8	24 ft.	531.8 ft.	534.5 ft.	6,005 ft.	-2.65 ft.
361	508	24 ft.	532.0 ft.	534.4 ft.	5,995 ft.	-2.35 ft.
362	510.9	24 ft.	534.9 ft.	535.2 ft.	6,079 ft.	-0.29 ft.
363	510.6	24 ft.	534.6 ft.	535.7 ft.	6,127 ft.	-1.07 ft.
364	510.2	24 ft.	534.2 ft.	536.0 ft.	6,160 ft.	-1.80 ft.
365	509.8	24 ft.	533.8 ft.	536.3 ft.	6,189 ft.	-2.49 ft.
366	509.4	24 ft.	533.4 ft.	536.5 ft.	6,212 ft.	-3.12 ft.
367	509	24 ft.	533.0 ft.	536.7 ft.	6,230 ft.	-3.70 ft.
368	509.1	24 ft.	533.1 ft.	537.0 ft.	6,256 ft.	-3.86 ft.
369	509.4	24 ft.	533.4 ft.	537.1 ft.	6,273 ft.	-3.73 ft.
370	509.4	24 ft.	533.4 ft.	537.3 ft.	6,290 ft.	-3.90 ft.
371	510	24 ft.	534.0 ft.	537.5 ft.	6,307 ft.	-3.47 ft.
372	510.3	24 ft.	534.3 ft.	537.7 ft.	6,325 ft.	-3.35 ft.
373	510.6	24 ft.	534.6 ft.	537.8 ft.	6,339 ft.	-3.19 ft.
374	510.9	24 ft.	534.9 ft.	537.9 ft.	6,350 ft.	-3.00 ft.
375	511.2	24 ft.	535.2 ft.	538.0 ft.	6,361 ft.	-2.81 ft.
376	511.5	24 ft.	535.5 ft.	538.2 ft.	6,376 ft.	-2.66 ft.
377	511.8	24 ft.	535.8 ft.	538.4 ft.	6,396 ft.	-2.56 ft.
378	512	24 ft.	536.0 ft.	538.6 ft.	6,415 ft.	-2.55 ft.
379	511.7	24 ft.	535.7 ft.	539.7 ft.	6,525 ft.	-3.95 ft.
380	511.2	24 ft.	535.2 ft.	539.5 ft.	6,512 ft.	-4.32 ft.
381	510.9	24 ft.	534.9 ft.	539.3 ft.	6,493 ft.	-4.43 ft.
382	510.6	24 ft.	534.6 ft.	539.2 ft.	6,475 ft.	-4.55 ft.
383	510.3	24 ft.	534.3 ft.	539.0 ft.	6,457 ft.	-4.67 ft.
384	510	24 ft.	534.0 ft.	538.8 ft.	6,440 ft.	-4.80 ft.
385	509.7	24 ft.	533.7 ft.	538.6 ft.	6,423 ft.	-4.93 ft.
386	509.4	24 ft.	533.4 ft.	538.5 ft.	6,406 ft.	-5.06 ft.
387	509.1	24 ft.	533.1 ft.	538.3 ft.	6,390 ft.	-5.20 ft.

<i>Lot No.</i>	<i>Pad Elevation</i>	<i>P.D.D. Building Height (1 Story)</i>	<i>Building Height Elevation</i>	<i>F.A.A. Threshold for Review</i>	<i>Distance from End of Runway</i>	<i>Difference to Threshold</i>
<b>Runway Elevation =</b>				<b>474.4</b>		
388	509.1	24 ft.	533.1 ft.	537.9 ft.	6,353 ft.	-4.83 ft.
389	509.4	24 ft.	533.4 ft.	537.7 ft.	6,334 ft.	-4.34 ft.
390	509.7	24 ft.	533.7 ft.	537.5 ft.	6,312 ft.	-3.82 ft.
391	510	24 ft.	534.0 ft.	537.3 ft.	6,285 ft.	-3.25 ft.
392	510.3	24 ft.	534.3 ft.	536.9 ft.	6,254 ft.	-2.64 ft.
393	510.6	24 ft.	534.6 ft.	536.6 ft.	6,220 ft.	-2.00 ft.
394	510.9	24 ft.	534.9 ft.	536.3 ft.	6,188 ft.	-1.38 ft.
395	514.5	18 ft.	532.5 ft.	536.5 ft.	6,208 ft.	-3.98 ft.
396	514.1	18 ft.	532.1 ft.	537.4 ft.	6,296 ft.	-5.26 ft.
397	513.6	18 ft.	531.6 ft.	538.2 ft.	6,376 ft.	-6.56 ft.
398	513.1	18 ft.	531.1 ft.	539.0 ft.	6,456 ft.	-7.86 ft.
399	514.2	24 ft.	538.2 ft.	540.3 ft.	6,586 ft.	-2.06 ft.
400	514	24 ft.	538.0 ft.	540.3 ft.	6,585 ft.	-2.25 ft.
401	513.7	24 ft.	537.7 ft.	540.3 ft.	6,585 ft.	-2.55 ft.
402	513.4	24 ft.	537.4 ft.	540.2 ft.	6,583 ft.	-2.83 ft.
403	513.1	24 ft.	537.1 ft.	540.2 ft.	6,579 ft.	-3.09 ft.
404	512.7	24 ft.	536.7 ft.	540.1 ft.	6,573 ft.	-3.43 ft.
405	512.4	24 ft.	536.4 ft.	540.0 ft.	6,564 ft.	-3.64 ft.
406	512.1	24 ft.	536.1 ft.	540.0 ft.	6,556 ft.	-3.86 ft.
407	511.8	24 ft.	535.8 ft.	539.9 ft.	6,550 ft.	-4.10 ft.
408	511.5	24 ft.	535.5 ft.	539.9 ft.	6,547 ft.	-4.37 ft.
409	511.5	24 ft.	535.5 ft.	539.9 ft.	6,552 ft.	-4.42 ft.
410	511.8	24 ft.	535.8 ft.	540.0 ft.	6,560 ft.	-4.20 ft.
411	512.1	24 ft.	536.1 ft.	540.1 ft.	6,570 ft.	-4.00 ft.
412	512.4	24 ft.	536.4 ft.	540.2 ft.	6,583 ft.	-3.83 ft.
413	512.7	24 ft.	536.7 ft.	540.4 ft.	6,599 ft.	-3.69 ft.
414	513.1	24 ft.	537.1 ft.	540.6 ft.	6,622 ft.	-3.52 ft.
415	513.4	24 ft.	537.4 ft.	541.2 ft.	6,675 ft.	-3.75 ft.
416	512.4	24 ft.	536.4 ft.	541.6 ft.	6,716 ft.	-5.16 ft.
417	512	24 ft.	536.0 ft.	541.4 ft.	6,704 ft.	-5.44 ft.
418	511.7	24 ft.	535.7 ft.	541.3 ft.	6,694 ft.	-5.64 ft.
419	511.4	24 ft.	535.4 ft.	541.3 ft.	6,688 ft.	-5.88 ft.
420	511.6	24 ft.	535.6 ft.	541.3 ft.	6,687 ft.	-5.67 ft.
421	511.9	24 ft.	535.9 ft.	541.3 ft.	6,690 ft.	-5.40 ft.
422	512.3	24 ft.	536.3 ft.	541.4 ft.	6,697 ft.	-5.07 ft.
423	512.6	24 ft.	536.6 ft.	541.5 ft.	6,706 ft.	-4.86 ft.
424	512.9	24 ft.	536.9 ft.	541.5 ft.	6,714 ft.	-4.64 ft.
425	513.1	24 ft.	537.1 ft.	541.6 ft.	6,719 ft.	-4.49 ft.
426	513.4	24 ft.	537.4 ft.	541.6 ft.	6,723 ft.	-4.23 ft.

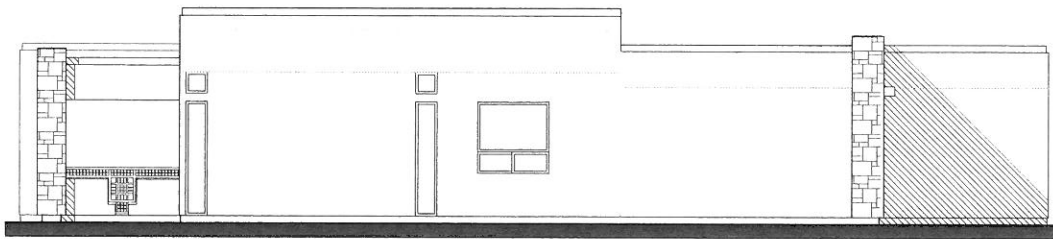


<i>Lot No.</i>	<i>Pad Elevation</i>	<i>P.D.D. Building Height (1 Story)</i>	<i>Building Height Elevation</i>	<i>F.A.A. Threshold for Review</i>	<i>Distance from End of Runway</i>	<i>Difference to Threshold</i>
<b>Runway Elevation =</b>				<b>474.4</b>		
427	513.7	24 ft.	537.7 ft.	541.6 ft.	6,724 ft.	-3.94 ft.
428	514	24 ft.	538.0 ft.	541.6 ft.	6,724 ft.	-3.64 ft.
429	514.3	24 ft.	538.3 ft.	541.6 ft.	6,724 ft.	-3.34 ft.

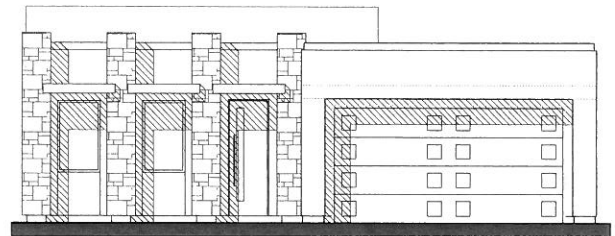
# Palm Springs Country Club



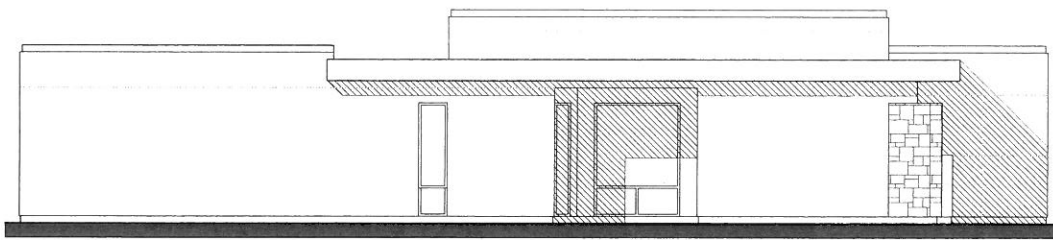
Front Elevation



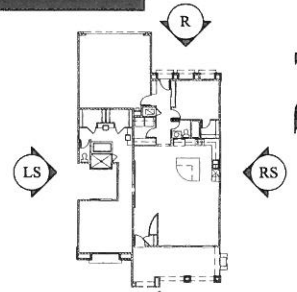
Right Side Elevation



Rear Elevation




Left Side Elevation



Plan 1a 1657 s.f.



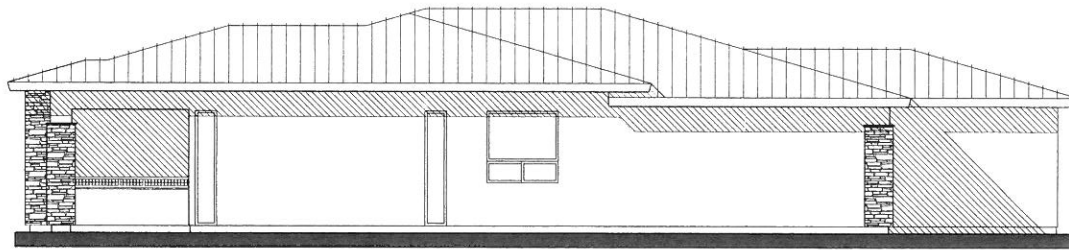
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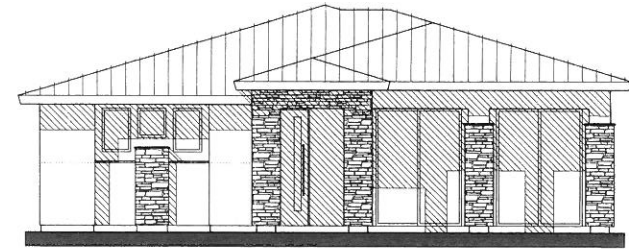
**Toblesky  
Green  
Architects**  
Incorporated  
P.O. Box 1261  
Chino Hills, CA  
91709  
(909) 393-2754

DATE	DESCRIPTION	BY
8-1-2014	RESPONSE TO RFP LETTER	

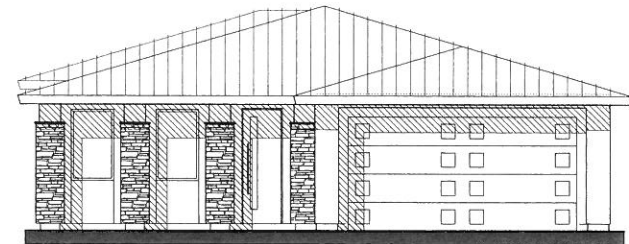
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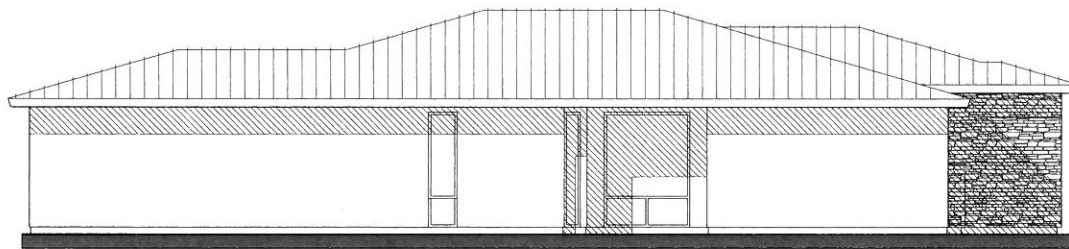
Right Side Elevation



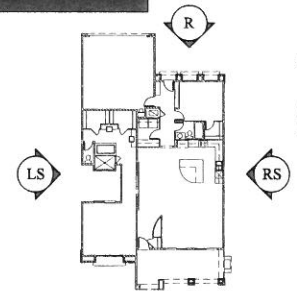
Front Elevation



Rear Elevation

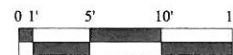


Left Side Elevation



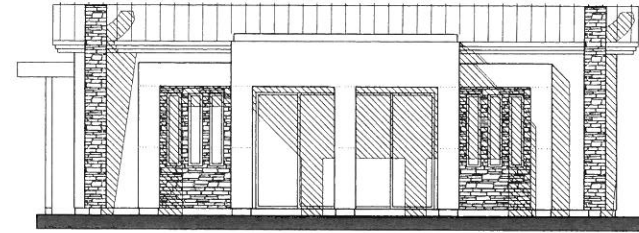
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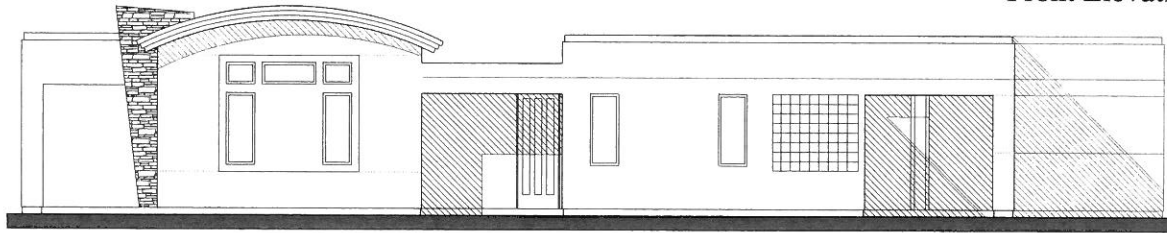


**Toblesky Green Architects**  
 Incorporated  
 P.O. Box 1261  
 Chino Hills, CA  
 91709  
 (909) 393-2754

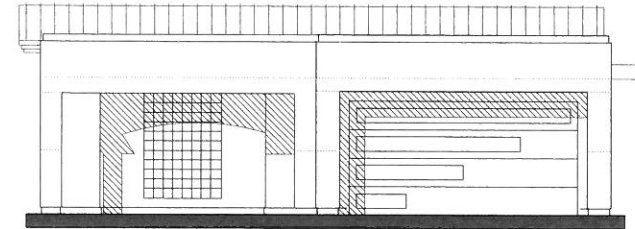
DATE	DESCRIPTION	BY
6-1-2014	RESPONSE TO RFP LETTER	



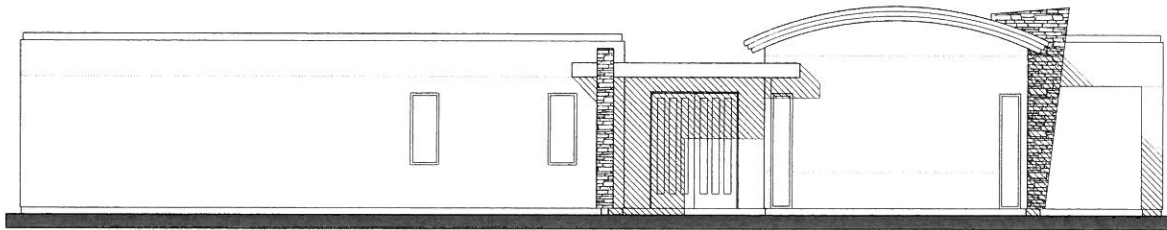
Front Elevation



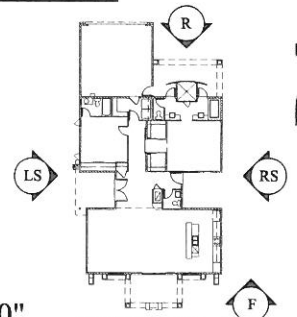
Right Side Elevation



Rear Elevation



Left Side Elevation



Plan 2a 1760 s.f.

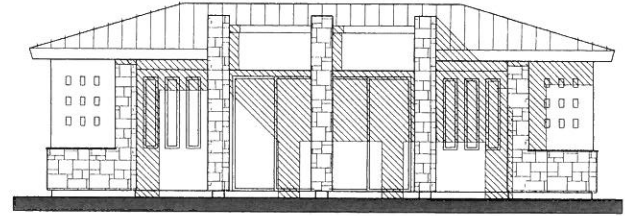


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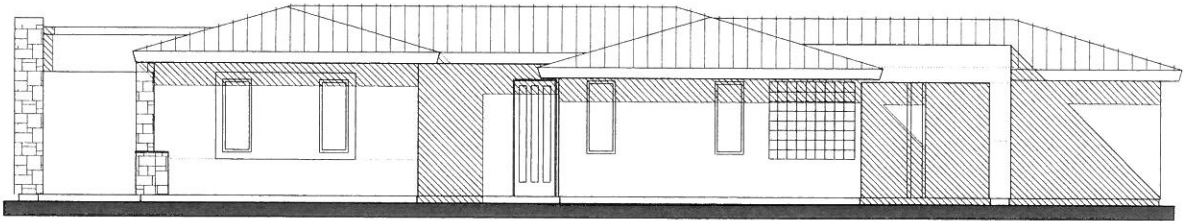
**Toblesky Green Architects**  
 Incorporated  
 P.O. Box 1261  
 Chino Hills, CA  
 91709  
 (909) 355-2754

DATE	DESCRIPTION
8-1-2014	3/12/14 RESPONSE TO 30214 LETTER

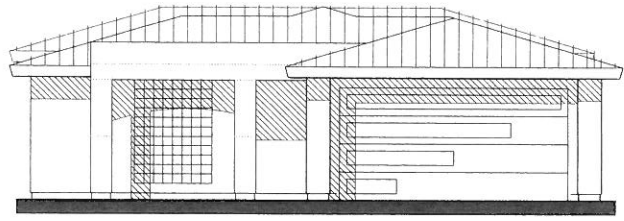
**Palm Springs Country Club**



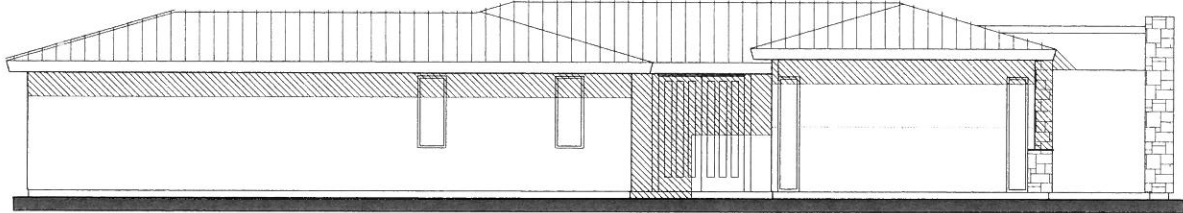
**Front Elevation**



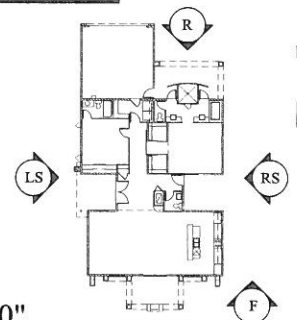
**Right Side Elevation**



**Rear Elevation**



**Left Side Elevation**



**Plan 2b 1760 s.f.**



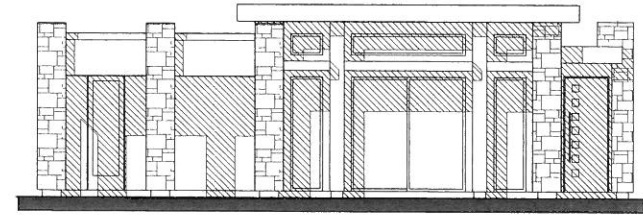
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**Toblesky Green Architects**  
 Incorporated  
 P.O. Box 1261  
 Chino Hills, CA  
 91709  
 (909) 352-2754

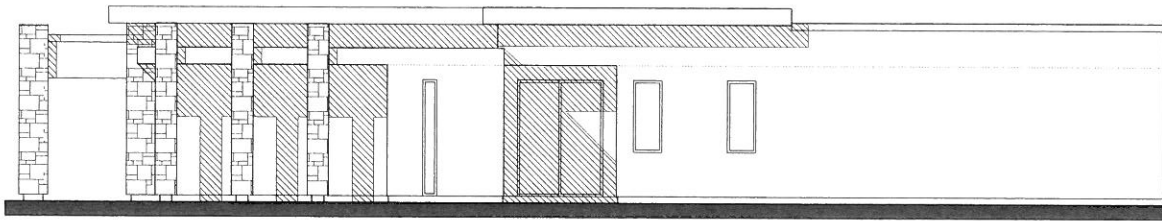
3/21/14 RESPONSE TO 329/14 LETTER

8-1-2014	

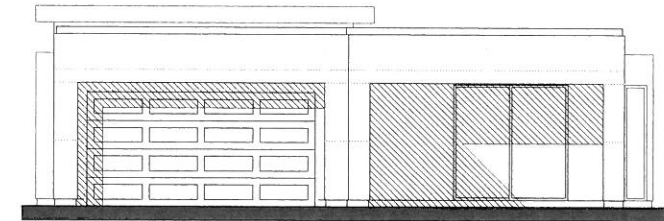
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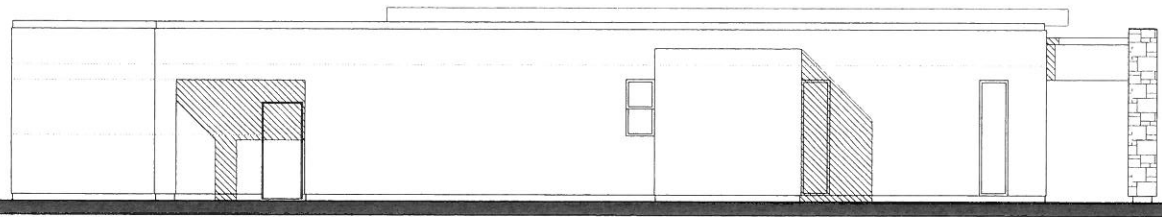
Front Elevation



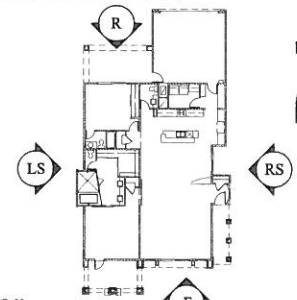
Right Side Elevation



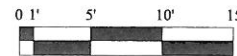
Rear Elevation



Left Side Elevation



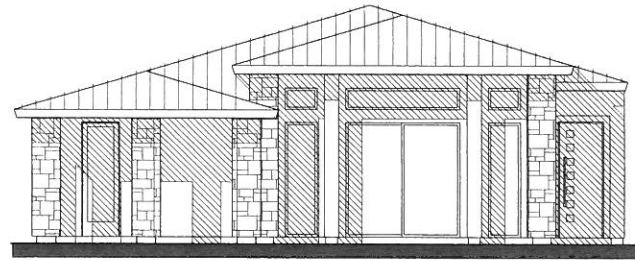
Plan 3a 1918 s.f.



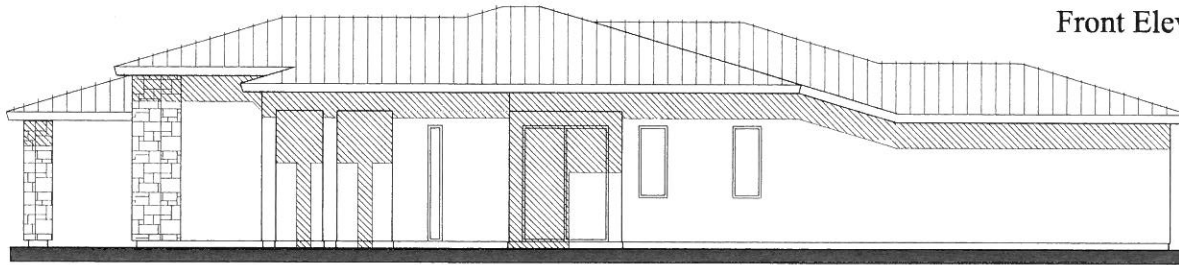
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**Toblesky  
Green  
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P.O. Box 1261  
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91709  
(909) 393-2754

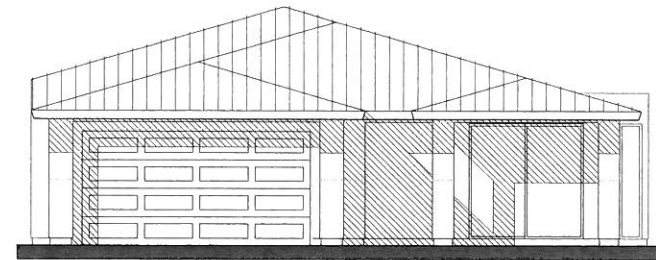
DATE	DESCRIPTION	BY	CHKD
8-1-2014	REVISION		



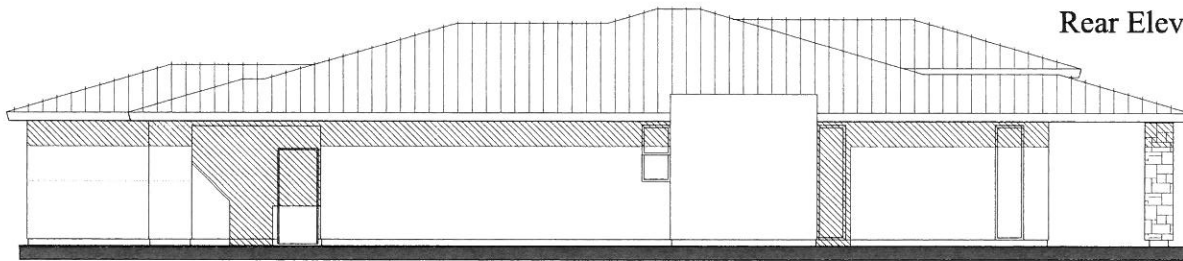
Front Elevation



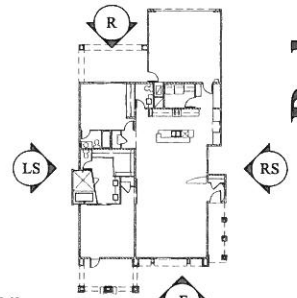
Right Side Elevation



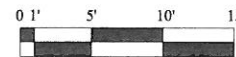
Rear Elevation



Left Side Elevation



Plan 3b 1918 s.f.

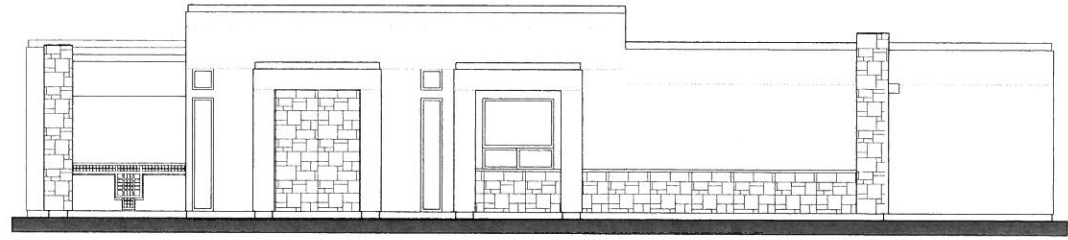


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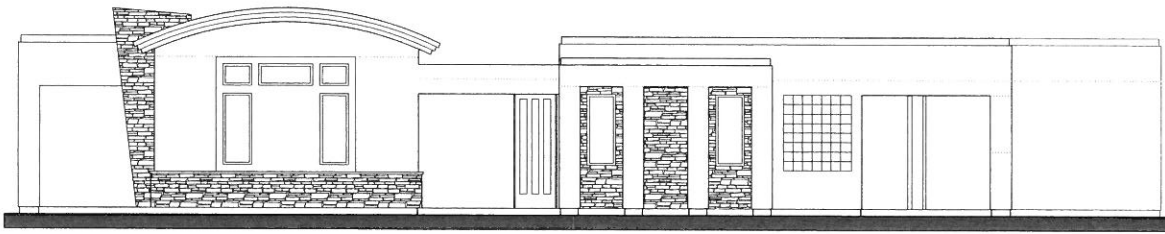
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 Chino Hills, CA  
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5/14 RESPONSE TO SOUTH LETTER	
5-1-2014	

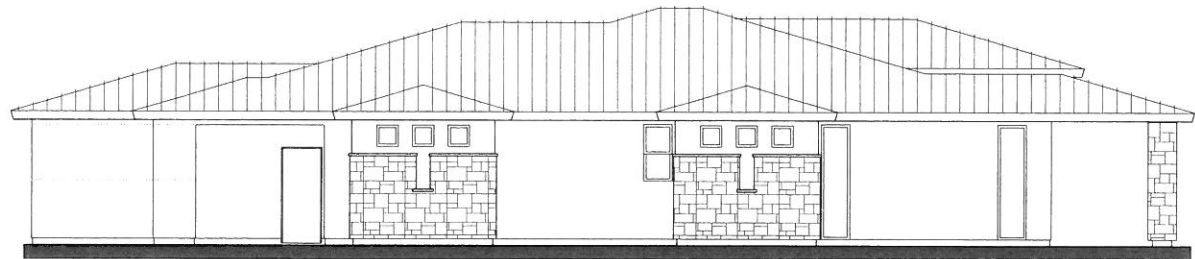
A1-12



Street 'A' Plan 1 Enhanced Side Elevation



Street 'A' Plan 2 Enhanced Side Elevation



Street 'A' Plan 3 Enhanced Side Elevation

Sample of Enhanced Side Elevations

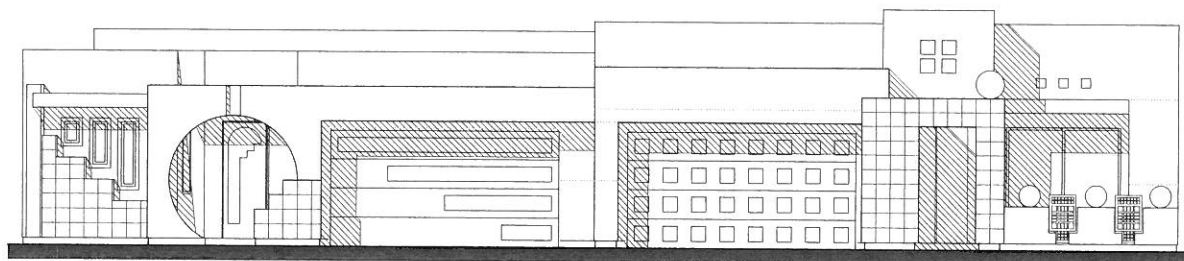
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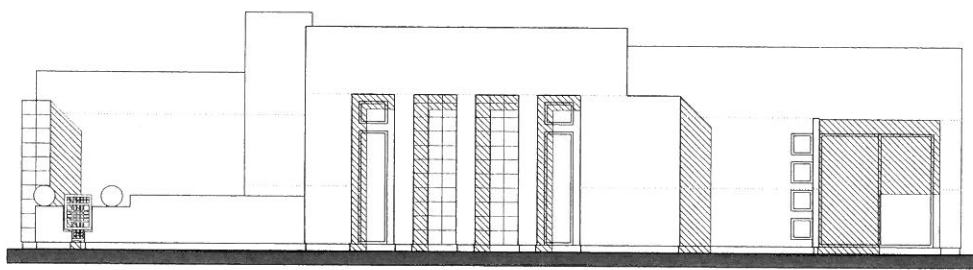
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NSIP14 RESPONSE TO 2014 LETTER	
8-1-2014	

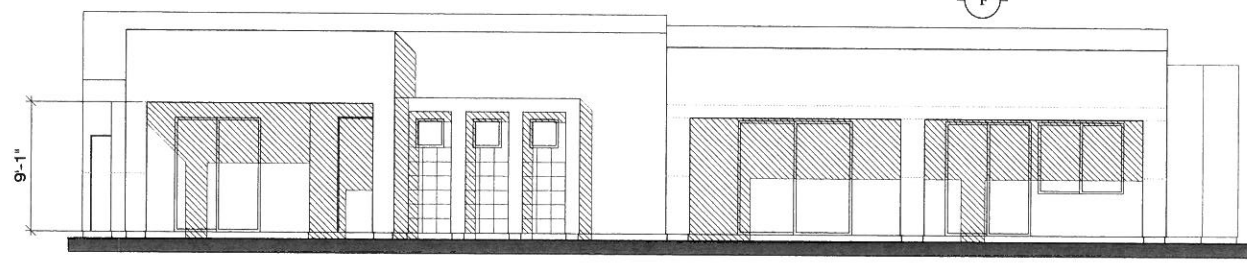
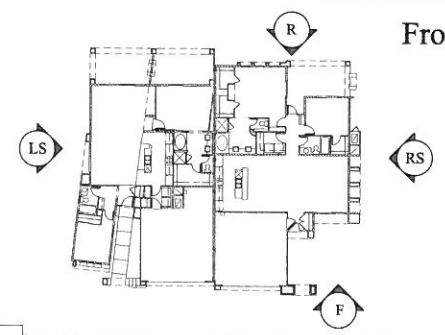




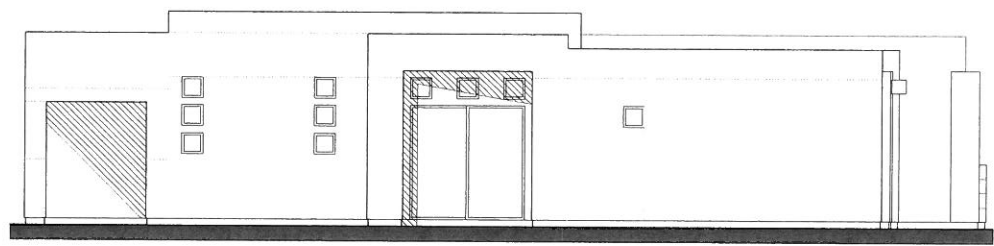
Front Elevation



Right Side Elevation

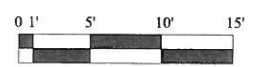


Rear Elevation



Left Side Elevation

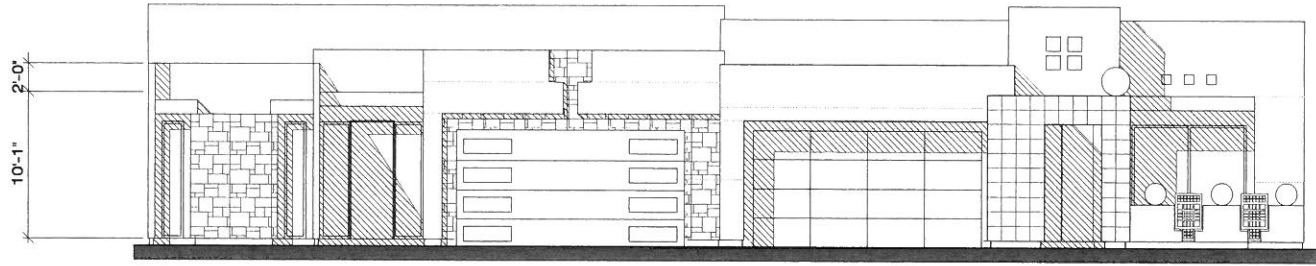
Plan 1 1342 s.f. & Plan 2 1432 s.f.



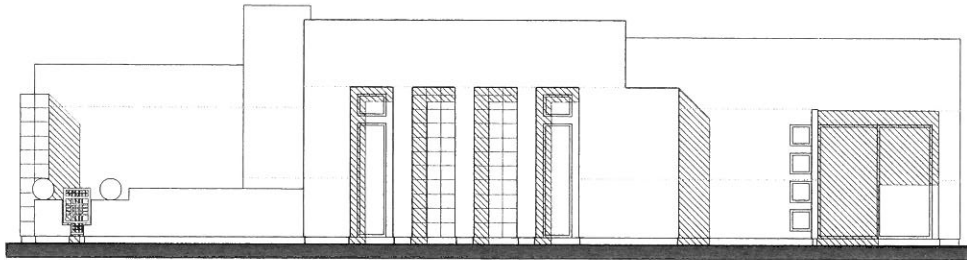
Scale 1/4"=1'-0"

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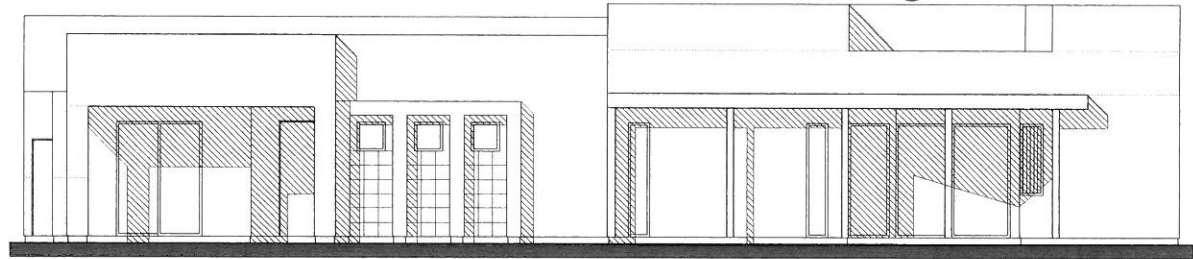
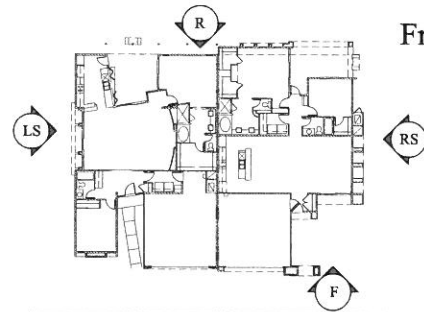
SYMBOL	RESPONSE TO 2004 LETTER



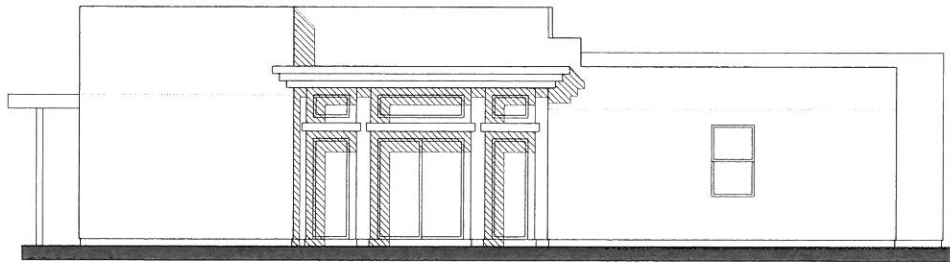
Front Elevation



Right Side Elevation

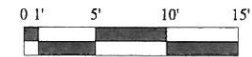


Rear Elevation

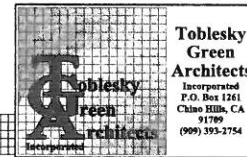


Left Side Elevation

Plan 3 1534 s.f. & Plan 2 1432 s.f.

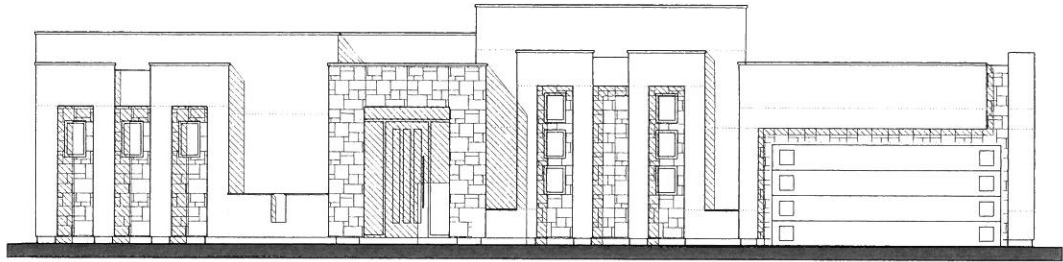


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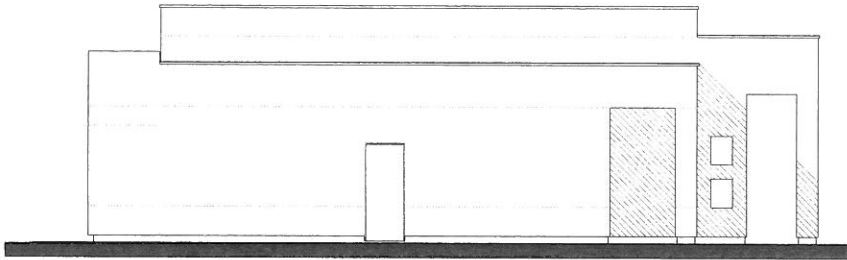


SHEET RESPONSE TO BIDDING LETTER	

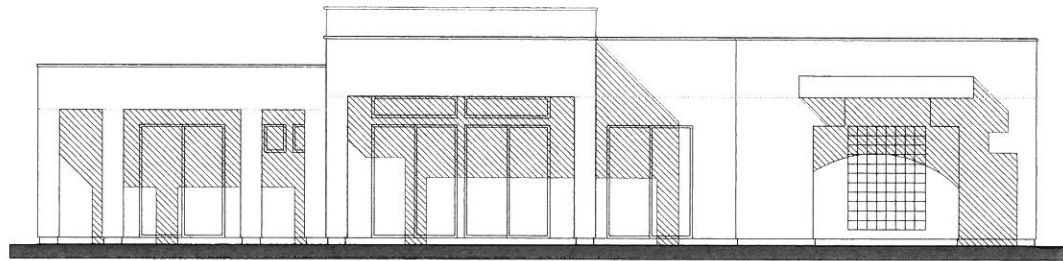
# Palm Springs Country Club



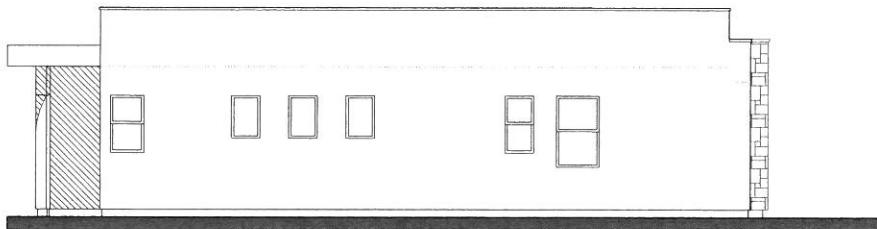
Front Elevation



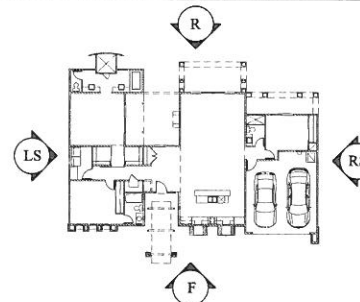
Right Side Elevation



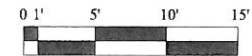
Rear Elevation



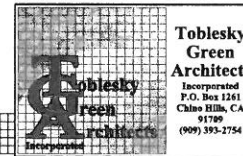
Left Side Elevation



Plan 1a 1956 s.f.



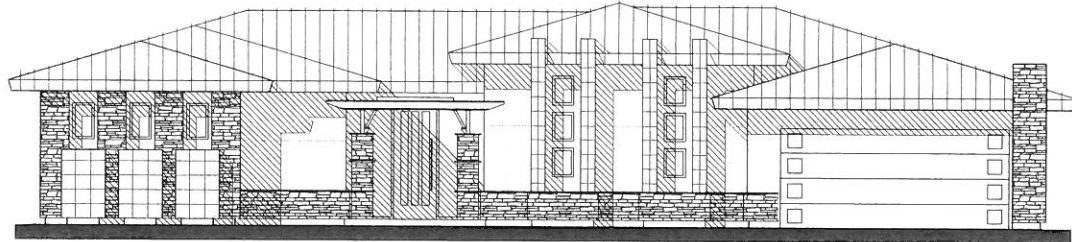
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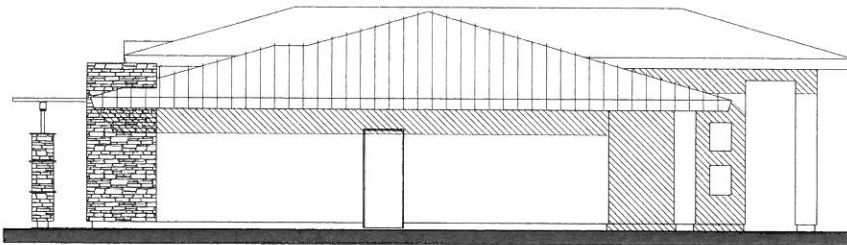
**Toblesky  
Green  
Architects**  
Incorporated  
P.O. Box 1261  
Chino Hills, CA  
91709  
(909) 393-2754

DATE	DESCRIPTION	BY	CHKD
8-1-2014	REVISED TO 2014 LETTER		

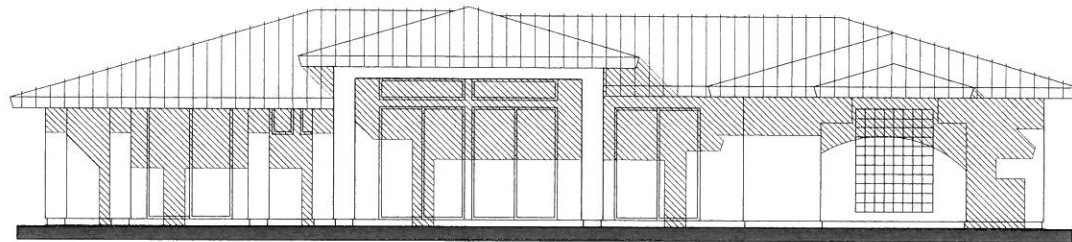
# Palm Springs Country Club



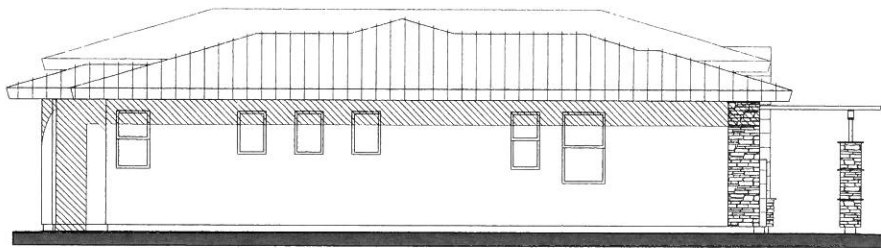
Front Elevation



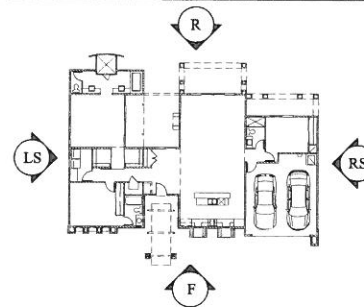
Right Side Elevation



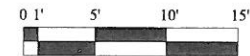
Rear Elevation



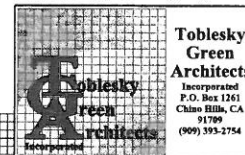
Left Side Elevation



Plan 1b 1956 s.f.



Scale 1/4"=1'-0"

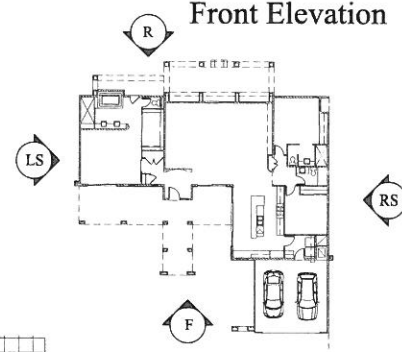


DATE	DESCRIPTION	BY
8-1-2014	RESPONSE TO 20014 LETTER	

**A3-6**

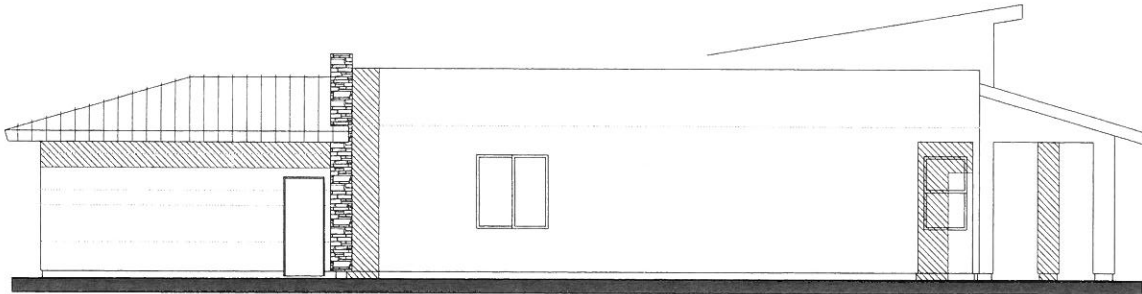
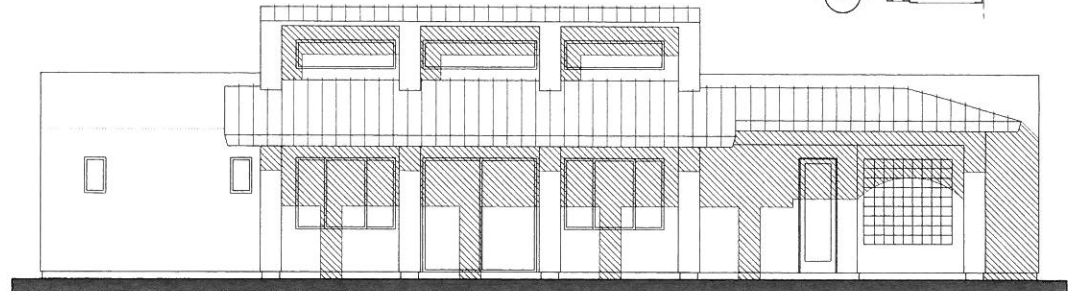


Front Elevation

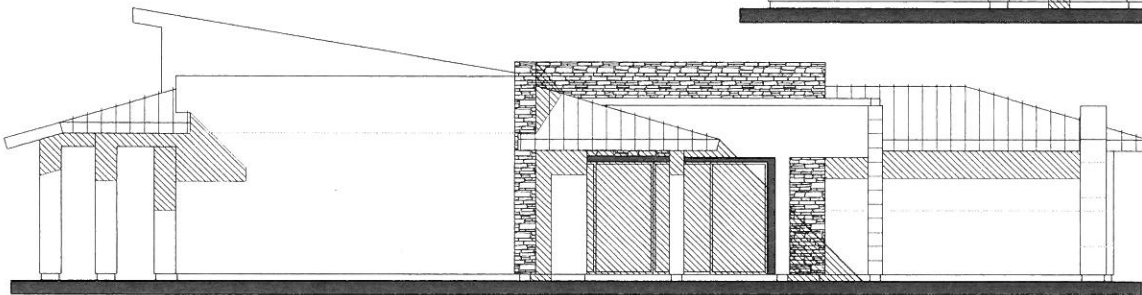


Plan 2a 2259 s.f.

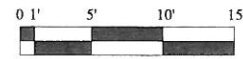
Rear Elevation



Right Side Elevation



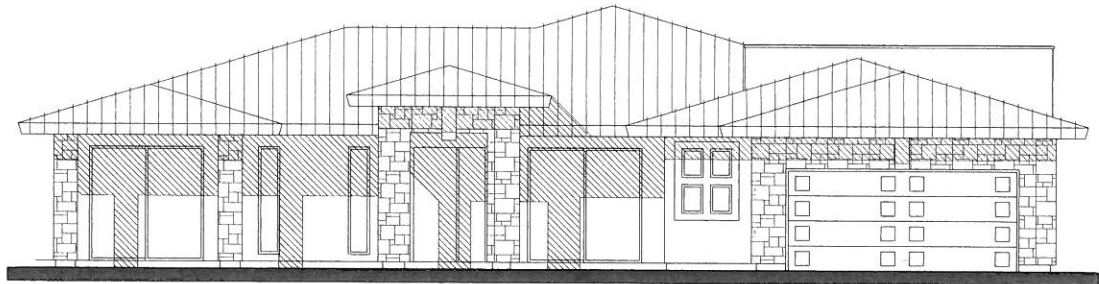
Left Side Elevation



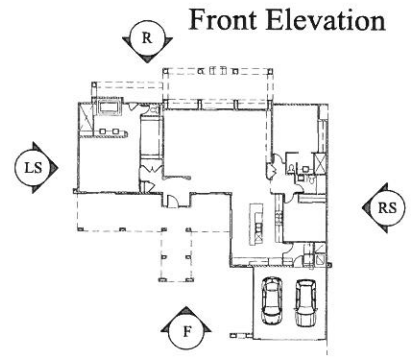
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DATE	DESCRIPTION	BY	CHKD
8-1-2014	RESPONSE TO 2014 LETTER		

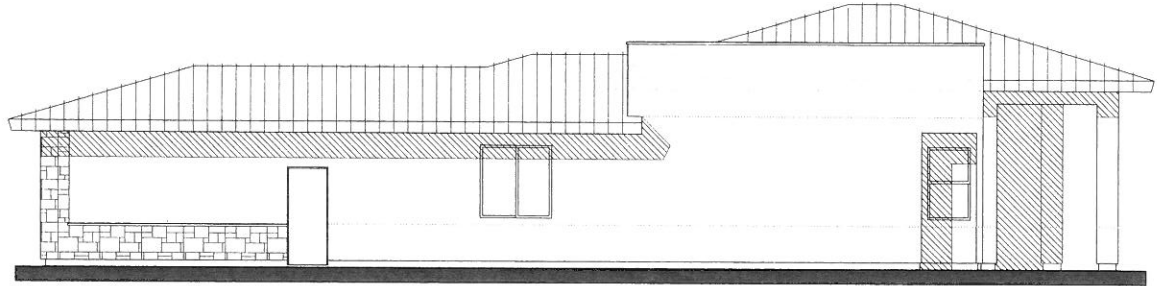


Front Elevation

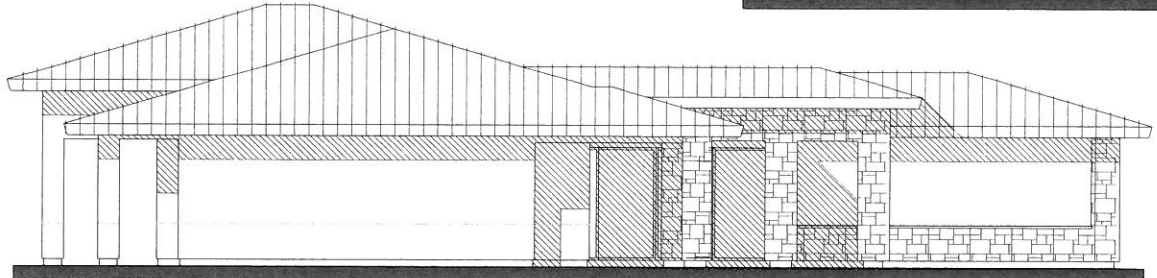
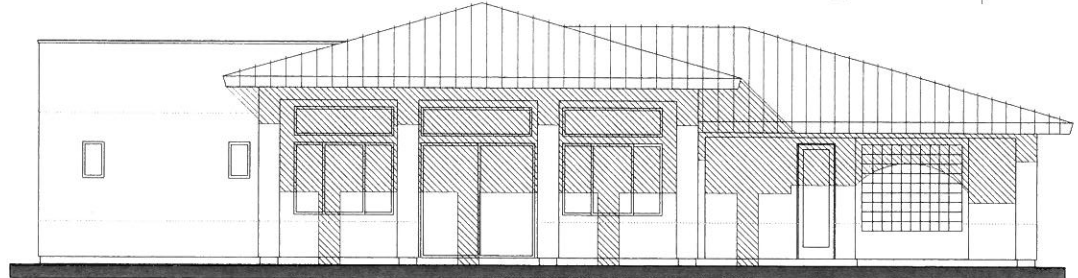


Plan 2b 2259 s.f.

Rear Elevation



Right Side Elevation



Left Side Elevation

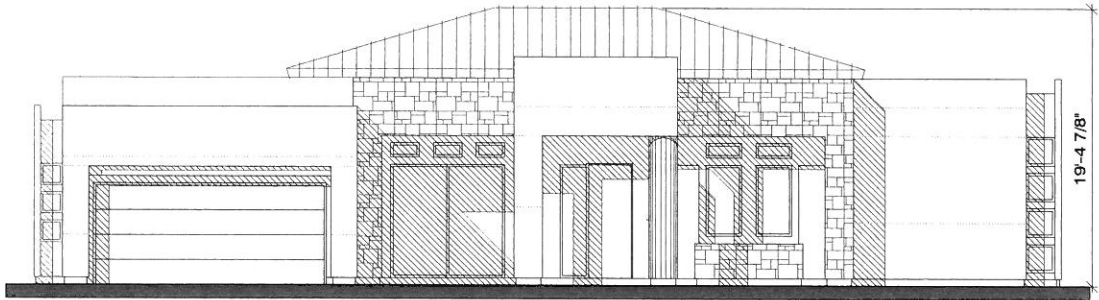


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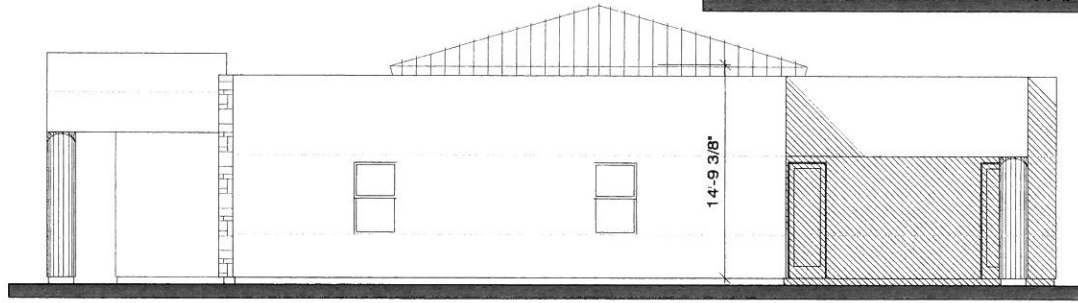
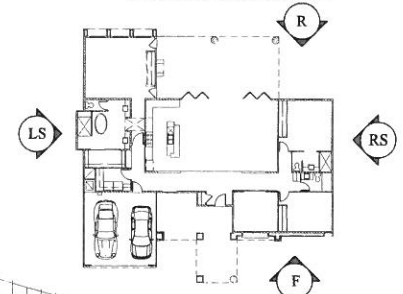
**Toblesky Green Architects**  
Incorporated  
P.O. Box 1261  
Chico Hills, CA  
91789  
(909) 393-2754

DATE	DESCRIPTION	BY
8-1-2014		

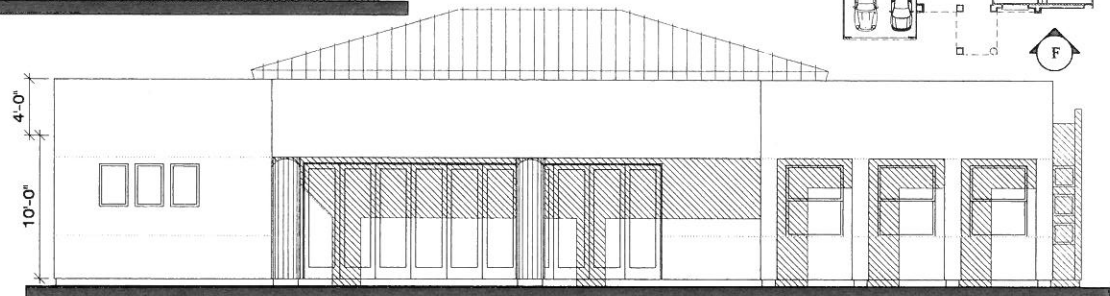
# Palm Springs Country Club



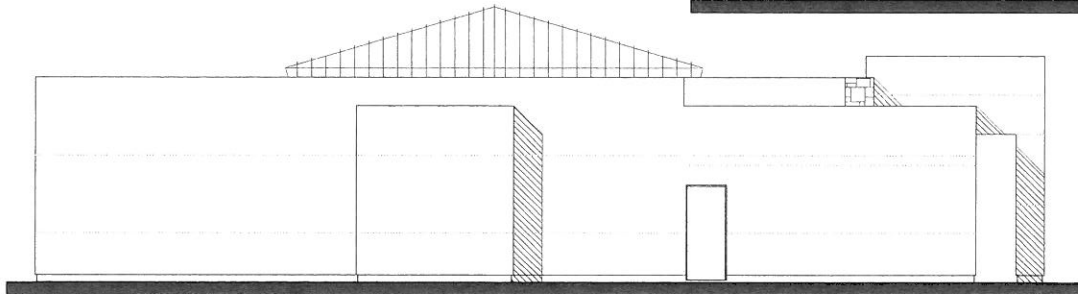
Front Elevation



Right Side Elevation



Rear Elevation

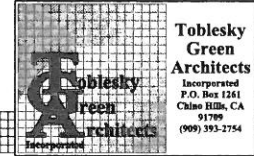


Left Side Elevation

Plan 3a 2524 s.f.



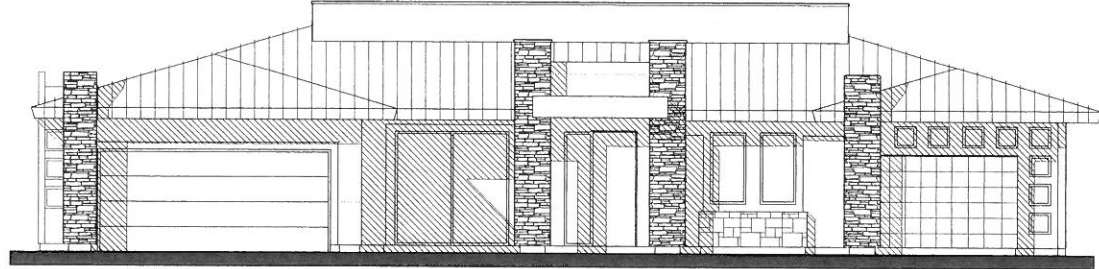
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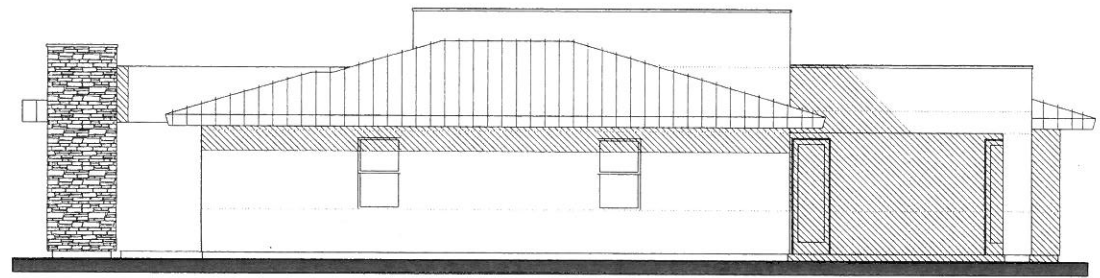
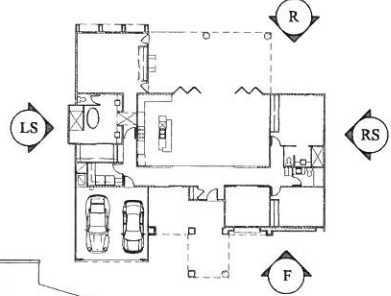
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DATE	DESCRIPTION	BY
8-1-2014	ISSUE RESPONSE TO 3024 LETTER	

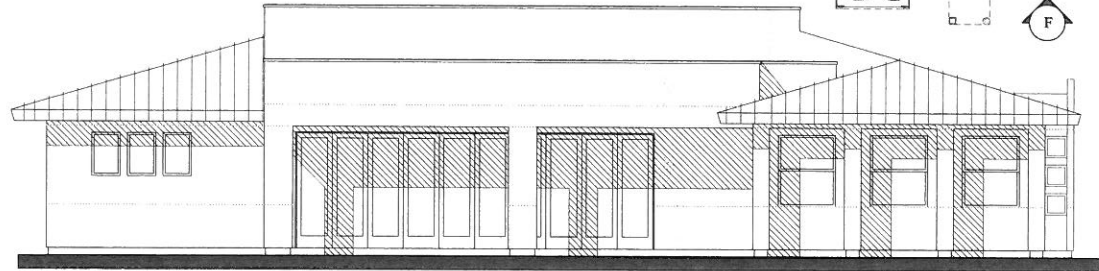
Palm Springs Country Club



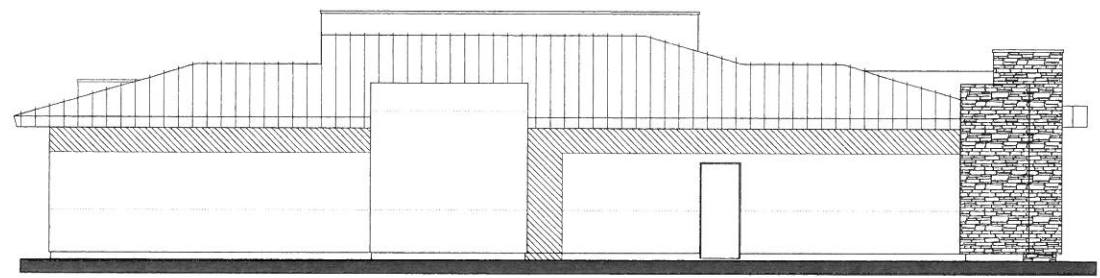
Front Elevation



Right Side Elevation



Rear Elevation



Left Side Elevation

Plan 3b 2524 s.f.



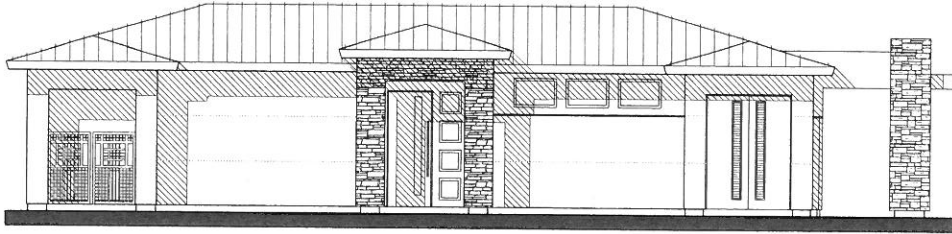
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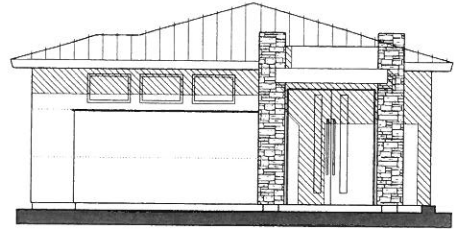
DATE	DESCRIPTION	BY
8-1-2014	3/13/14 RESPONSE TO 2014 LETTER	

A3-18

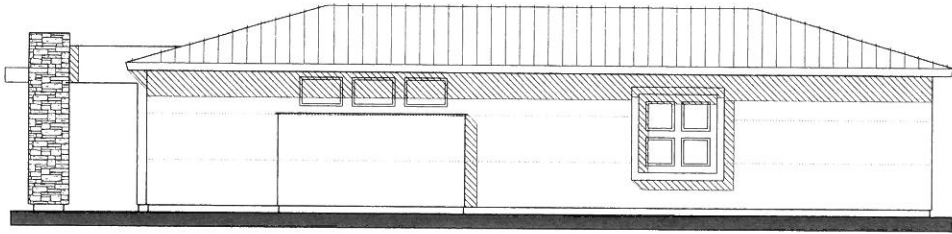




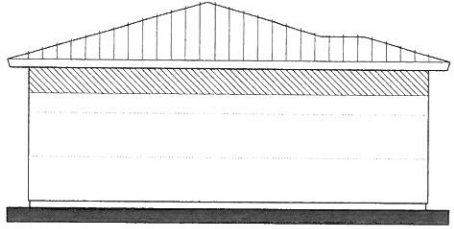
Left Side Elevation



Front Elevation



Right Side Elevation



Rear Elevation

Clubhouse



Scale 1/4"=1'-0"

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 Chino Hills, CA  
 91789  
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3/12/14 RESPONSE TO 2014 LETTER	_____
8-1-2014	_____
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## PS. PALM SPRINGS INTERNATIONAL AIRPORT

### PS.1 Compatibility Map Delineation

- 1.1 *Airport Master Plan Status:* The *Airport Master Plan* adopted by the Palm Springs City Council in 2002 is the basis for the *Compatibility Plan*.
- 1.2 *Airfield Configuration:* Establishment of a precision instrument approach procedure on Runway 31L is proposed, but no other runway system changes are indicated in the *Master Plan*.
- 1.3 *Airport Activity:* Despite a projected increase from 109,500 aircraft operations in 2002 to 170,260 in 2020, the *Master Plan* anticipates Palm Springs International Airport noise contours to slightly shrink in most locations. This impact reduction reflects the reduced single-event noise levels produced by the aircraft that will make up the future fleet mix at the airport compared to those operating there today. For the purposes of the *Compatibility Plan*, a composite of the 2002 and 2020 noise contours is used.
- 1.4 *Airport Influence Area:* The locations of the standard flight paths flown by aircraft approaching and departing the airport are the primary factors defining the influence area for Palm Springs International Airport. Close-in areas west of the airport are affected by sideline noise, but the more distant areas are seldom overflown and thus are excluded from the airport influence area.

### PS.2 Additional Compatibility Policies

- 2.1 *Noise Exposure in Residential Areas:* The limit of 60 dB CNEL set by Countywide Policy 4.1.4 as the maximum noise exposure considered normally acceptable for new residential land uses shall not be applied to the environs of Palm Springs International Airport. For this airport, the criterion shall instead be 62 dB CNEL. This higher threshold takes into account the ambient noise conditions in the area and also the community's long-standing exposure to the noise of airline aircraft operations. Dwellings may require incorporation of special noise level reduction measures into their design to ensure that the interior noise limit of 45 dB CNEL (Countywide Policy 4.1.6) is not exceeded.
- 2.2 *Zone C Residential Densities:* The criteria set forth in Countywide Policy 3.1.3(a) and the Basic Compatibility Criteria matrix (Table 2A) notwithstanding, residential densities in Zone C northwest of the airport shall either be kept to a very low density of no more than 0.2 dwelling units per acre as indicated in the table or be in the range of 3.0 to 15.0 dwelling units per acre. The choice between these two options is at the discretion of the City of Palm Springs, the only affected land use jurisdictions. (Criteria for Zone C southeast of the airport remain as indicated in Table 2A.)
- 2.3 *Zone D Residential Densities:* The criteria set forth in Countywide Policy 3.1.3(b) and the Basic Compatibility Criteria matrix (Table 2A) notwithstanding, the high-density option for *Compatibility Zone D* at Palm Springs International Airport shall

allow residential densities as low as 3.0 dwelling units per gross acre to the extent that such densities are typical of existing (as of the adoption date of this plan) residential development in nearby areas of the community.

2.4 *Southeast Industrial/Commercial Area*: Within the areas designated by a (1) and a (2) on the Palm Springs International Airport Compatibility Map, the following usage intensity criteria shall apply:

(a) In *Compatibility Zone B1*:

- (1) An average of up to 40 people per acre shall be allowed on a site and up to 80 people shall be allowed to occupy any single acre of the site.
- (2) If the percentage of qualifying open land on the site (see Countywide Policy 4.2.4) is increased from 30% to at least 35%, the site shall be allowed to have an average of up to 45 people per acre and any single acre shall be allowed to have up to 90 people per acre.
- (3) If the percentage of qualifying open land on the site is increased to 40% or more, the site shall be allowed to have an average of up to 50 people per acre and any single acre shall be allowed to have up to 100 people per acre.

(b) In *Compatibility Zone C*:

- (1) An average of up to 80 people per acre shall be allowed on a site and up to 160 people shall be allowed to occupy any single acre of the site.
- (2) If the percentage of qualifying open land on the site is increased from 20% to at least 25%, the site shall be allowed to have an average of up to 90 people per acre and any single acre shall be allowed to have up to 180 people per acre.
- (3) If the percentage of qualifying open land on the site is increased to 30% or more, the site shall be allowed to have an average of up to 100 people per acre and any single acre shall be allowed to have up to 200 people per acre.

(c) To the extent feasible, open land should be situated along the extended runway centerlines or other primary flight tracks.

(d) The above bonuses for extra open land on a site are in addition to the intensity bonuses for risk-reduction building design indicated in Table 2A. In both cases, incorporation of the features necessary to warrant the intensity bonuses is at the option of the City of Palm Springs and the project proponents and is not required by ALUC policy.

(e) The intensity bonuses for extra open land provided here are judged to represent a balance between the ALUC objective of enhancing safety in the airport environs and needs of the community for more intensive development of the area involved. The resulting intensities remain consistent with the guidelines set in the *California Airport Land Use Planning Handbook* given the character of the airport activity and the surrounding community.

2.5 *Expanded Buyer Awareness Measures*: In addition to the requirements for aviation easement dedication or deed notification as indicated in Table 2A, any new single-

family or multi-family residential development proposed for construction anywhere within the Palm Springs International Airport influence area, except for *Compatibility Zone E*, shall include the following measures intended to ensure that prospective buyers or renters are informed about the presence of aircraft overflights of the property.

- (a) During initial sales of properties within newly created subdivisions, large airport-related informational signs shall be installed and maintained by the developer. These signs shall be installed in conspicuous locations and shall clearly depict the proximity of the property to the airport and aircraft traffic patterns.
- (b) An informational brochure shall be provided to prospective buyers or renters showing the locations of aircraft flight patterns. The frequency of overflights, the typical altitudes of the aircraft, and the range of noise levels that can be expected from individual aircraft overflights shall be described.

# **NOTICE OF PUBLIC HEARING**

## **RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14<sup>th</sup> Floor, Riverside, California 92501, Monday through Thursday, from 8:00 a.m. to 5:00 p.m., and by prescheduled appointment on Fridays, from 9:00 a.m. to 5:00 p.m.

PLACE OF HEARING: Riverside County Administration Center  
4080 Lemon St., 1<sup>st</sup> Floor Hearing Room  
Riverside, California

DATE OF HEARING: April 9, 2015

TIME OF HEARING: 9:00 A.M.

### **CASE DESCRIPTION:**

ZAP1025PS15 – PS Country Club, LLC (Representative: Marvin Roos, MSA Consulting, Inc.) – City of Palm Springs Case Nos. 5.1327, PD-366, and Tentative Tract Map No. 36691 “Serena Park”. The applicant proposes to develop 429 single-family residential dwellings within the area that was formerly the Palm Springs Country Club golf course. Tentative Tract Map No. 36691 would divide 131.25 acres located northerly of Verona Road, easterly of Sunrise Way, and southwesterly of the Whitewater River Channel into 429 residential lots (292 detached units and 137 attached units), 5.39 acres of public open space, and 25 private open space lots. Case No. 5.1327 is a proposal to amend the General Plan designation of 125.86 acres from Open Space-Parks/Recreation (OS-P/R) to Very Low Density Residential (up to 4.0 dwelling units per acre) (VLDR). PD-366 is a proposal to place the same 125.86 acres in a Planned Development District, superseding the existing O and O5 open space zoning. (Airport Compatibility Zones C and D of the Palm Springs International Airport Influence Area).

FURTHER INFORMATION: Contact Russell Brady at (951) 955-0549 or John Guerin at (951) 955-0982. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Mr. David Newell of the City of Palm Springs Planning Department, at (760) 323-8245.

**APPLICATION FOR MAJOR LAND USE ACTION REVIEW**  
**RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**

ALUC Identification No.

ZAP1025PS15

**PROJECT PROPONENT** (TO BE COMPLETED BY APPLICANT)

Date of Application 02/04/15  
 Property Owner PS Country Club, LLC / Attn: Eric Taylor Phone Number 805-469-9510  
 Mailing Address 1601 Carmen Dr. Suite 211  
Camarillo CA 93010

Agent (if any) MSA Consulting, Inc. c/o Marvin Roos Phone Number 760-320-9811  
 Mailing Address 34200 Bob Hope Drive  
Rancho Mirage, CA 92270

**PROJECT LOCATION** (TO BE COMPLETED BY APPLICANT)

*Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways*

Street Address 2500 Whitewater Club Dr.  
Palm Springs, CA  
 Assessor's Parcel No. 501-190-011/669-480-027/669-590-066 Parcel Size 131.25 acres  
 Subdivision Name Serena Park Zoning  
 Lot Number \_\_\_\_\_ Classification O/OS Open Land Zone

**PROJECT DESCRIPTION** (TO BE COMPLETED BY APPLICANT)

*If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed*

Existing Land Use (describe) The project site is an abandoned 18 hole golf course formally known as The PS Country Club. The site is surrounded by existing residential lots and the Whitewater Flood Control levee. The project site has sparse vegetation with scattered palm trees.

Proposed Land Use (describe) The project proposes a TTM to redevelop the former golf course with approx. 429 residential units and a five-acre public park. These will consist of 137 single story, attached residences and 292 detached single-family residences. A GPA will is also proposed to change the GP from Open Space to Planned Development District (PDD).

For Residential Uses Number of Parcels or Units on Site (exclude secondary units) 429 Units  
 For Other Land Uses Hours of Use \_\_\_\_\_  
 (See Appendix C) Number of People on Site \_\_\_\_\_ Maximum Number \_\_\_\_\_  
 Method of Calculation \_\_\_\_\_

Height Data Height above Ground or Tallest Object (including antennas and trees) \_\_\_\_\_ 19 ft.  
 Highest Elevation (above sea level) of Any Object or Terrain on Site \_\_\_\_\_ 569 ft.

Flight Hazards Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?  Yes  
 No  
 If yes, describe \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_

C, D  
 P, S

REFERRING AGENCY (APPLICANT OR JURISDICTION TO COMPLETE)	
Date Received	<u>2-18-15</u>
Agency Name	<u>City of Palm Springs</u> <u>3200 E. Tahquitz Canyon Way, PS CA 92262</u>
Staff Contact	<u>David Newell, Associate Planner</u>
Phone Number	<u>760-323-8245</u>
Agency's Project No.	<u>Case 5.1327 PD-366 / TTM 36691</u>
Type of Project	<input checked="" type="checkbox"/> General Plan Amendment <input type="checkbox"/> Zoning Amendment or Variance <input type="checkbox"/> Subdivision Approval <input type="checkbox"/> Use Permit <input type="checkbox"/> Public Facility <input checked="" type="checkbox"/> Other <u>Tentative Tract Map</u>

**A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

**B. SUBMISSION PACKAGE:**

**ALUC REVIEW**

- 1 . . . . . Completed Application Form
- 1 . . . . . Project Site Plan – Folded (8-1/2 x 14 max.)
- 1 . . . . . Elevations of Buildings - Folded
- 1 Each . 8 ½ x 11 reduced copy of the above
- 1 . . . . . 8 ½ x 11 reduced copy showing project in relationship to airport.
- 1 Set . Floor plans for non-residential projects
- 4 Sets . Gummed address labels of the Owner and representative (**See Proponent**).
- 1 Set . Gummed address labels of all property owners within a 300' radius of the project site. If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10), with ALUC return address.
- 4 Sets . Gummed address labels of the referring agency (City or County).
- 1 . . . . . Check for Fee (See Item "C" below)

**STAFF REVIEW (Consult with ALUC staff planner as to whether project qualifies)**

- 1 . . . . . Completed Application Form
- 1 . . . . . Project Site Plans – Folded (8-1/2 x 14 max.)
- 1 . . . . . Elevations of Buildings - Folded
- 1 . . . . . 8 ½ x 11 Vicinity Map
- 1 Set . Gummed address labels of the Owner and representative (**See Proponent**).
- 1 Set . Gummed address labels of the referring agency.
- 1 . . . . . Check for review—See Below

**COUNTY OF RIVERSIDE  
AIRPORT LAND USE COMMISSION**

**STAFF REPORT**

**AGENDA ITEM:** 3.3

**HEARING DATE:** April 9, 2015

**CASE NUMBER:** ZAP1023FL14 – River Springs Charter School  
(Representative: Dave Black)

**APPROVING JURISDICTION:** City of Jurupa Valley

**JURISDICTION CASE NO:** MA 1492 (Public Use Permit)

**MAJOR ISSUES:** The proposed school does not comply with the average acre criterion of 100 people and the single-acre criterion of 300 for Compatibility Zone D based on the building code method. However, based on the applicant's planned maximum occupancy of 265 people, the school would comply with the criteria.

ALUC staff has discussed a posted occupancy of 265 people with the City of Jurupa Valley; however, their building official indicated that they would not enforce a maximum occupancy that is below the building code maximum occupancy. In-lieu of a posted occupancy, ALUC staff recommends the Commission consider proposed Condition 6 which would apply a general maximum occupancy through a maximum student enrollment of 250 and maximum staff of 15 that would be incorporated into the proposed Public Use Permit and enforced by City of Jurupa Valley Planning Department and/or Code Enforcement.

Additionally, children's schools are a discouraged use within Compatibility Zone D. However, the applicant has provided information on the school noting the importance of the location on the airport in its theme, programming, and existing charter.

**RECOMMENDATION:** Staff recommends a finding of INCONSISTENCY, based on the proposed project exceeding the Compatibility Zone D average and single-acre criteria. However, if the Commission is willing to accept recommended Condition 6 as sufficient to ensure occupancy would not exceed the Compatibility Zone D average and single-acre criteria, staff would recommend a finding of CONDITIONAL CONSISTENCY with the 2004 Flabob Airport Land Use Compatibility Plan, subject to a Determination by the FAA for the on-airport facility and any recommended conditions by the FAA and the conditions included herein.

**PROJECT DESCRIPTION:** The Public Use Permit proposes to relocate and establish Flabob Airport Preparatory Academy as a charter school, including the development of 17,690 square feet of building area (ten classrooms to serve elementary, secondary, and high school students and a 400



square foot administrative office), to be located in the northwesterly portion of the grounds of Flabob Airport. The Flabob Aviation Preparatory Academy currently occupies an existing building on the Flabob Airport property and has been operating since 2005.

**PROJECT LOCATION:** The main school site is located southwest of 42<sup>nd</sup> Street, southeast of Fort Drive, and northwest of Twining Street, within the City of Jurupa Valley, approximately 750 feet northerly of Runway 6-24 at Flabob Airport. The associated parking lots are located across 42<sup>nd</sup> Street.

**LAND USE PLAN:** 2004 Flabob Airport Land Use Compatibility Plan

- a. Airport Influence Area: Flabob Airport
- b. Land Use Policy: Airport Compatibility Zone D
- c. Noise Levels: Between 55-60 CNEL noise contour

**BACKGROUND:**

Non-Residential Average Intensity: The site is located in Compatibility Zone D. Zone D would limit average intensity to 100 people per acre. Based on the site plan and floor plan provided for the school, a total of 10,560 square feet of classroom and 960 square feet of office space are proposed. Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, the intensity of classroom areas is one person per 20 square feet and the intensity of office areas is one person per 100 square feet. However, Appendix C recommends that, for calculation of intensity levels, the Building Code occupancy levels be reduced by 50 percent, at least for office uses. Based on the area of uses (assuming a 50% reduction for office uses) and the number of people per square feet, a total of 533 people would be anticipated within the entire building. Based on the gross area of the site (2.73 gross acres), the average intensity would be 195 people per acre. Therefore, the proposed project would not be consistent with the Compatibility Zone D average acre criteria.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per standard vehicle in the absence of more precise data). Based on the number of parking spaces provided (105 standard vehicle spaces), the total occupancy would be estimated at 158 people for an average acre intensity of approximately 58, which is consistent with the Compatibility Zone D average acre intensity criteria. However, in the case of a school serving under-age pupils, the number of occupants cannot be reasonably projected utilizing the parking space method.

Despite the calculations pursuant to the building code method, the applicant is planning for a maximum of 25 students and 1 teacher per classroom and an additional 5 staff for a total planned occupancy of 265 people. Based on the applicant's proposed maximum of 265 people and the gross area of the site, the average intensity would be 97 people, which would be consistent with the

Compatibility Zone D average acre criteria. Pursuant to staff's recommended conditions, the total student enrollment would be limited to 250 and the total staff limited to 15 people.

Non-Residential Single-Acre Intensity: The site is located in Compatibility Zone D. Zone D would limit single-acre intensity to 300 people. However, with a gross area of 2.73 acres, the maximum allowable intensity would be 273. Based on the site plan and floor plan provided, the entire building would be located within a single-acre area (210' x 210'). As noted previously the building would accommodate a total of 533 people pursuant to the building code method, which would not be consistent with the Compatibility Zone D single-acre criteria. However, as noted by the applicant and as included in the recommended conditions of approval, the building can be limited to a maximum of 265 occupants, which would be consistent with the Compatibility Zone D single-acre criteria.

Risk-Reduction Design Bonus: Based on the site plan and floor plan provided, the proposed building is single story. The building includes windows for each of the classrooms and office area. No other information has been provided indicating whether any additional risk reduction design measures are or could be incorporated into the building design, such as concrete walls, upgraded roof strength, avoiding skylights, enhanced fire sprinkler, and increased emergency exits. Even if the maximum bonus of 390 were granted, the occupancy of 533 people per single-acre as calculated pursuant to the building code method would not comply.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited in Compatibility Zone D within the project. However, the project does propose a children's school, which is a discouraged use in Compatibility Zone D. As noted in a letter provided by the applicant, the school's educational programs would be centered around aviation to motivate and inspire students and that if the location were not on the airport this focus of the program would be lost. In addition, the applicant has indicated that their existing charter is associated with the location on the airport (as well as within the local community) and the aviation theming and that any location off the airport may jeopardize their existing charter. Therefore, although it is a children's school use, its programming and existing charter make the location on the airport important if not necessary.

Noise: The project site is within the 55-60 CNEL range from aircraft generated noise. ALUC's objective is that interior noise levels from aviation-related sources not exceed 40 dBA CNEL for noise sensitive land uses. As standard building construction is presumed to provide adequate sound attenuation where the exterior noise exposure is not more than 20 dB greater than the interior standard, the school building would not require special measures to mitigate aircraft-generated noise.

Part 77: As an on airport use, it is required to be submitted for review by Federal Aviation Administration (FAA) Obstruction Evaluation Service. A submittal has been made and been assigned Aeronautical Study Numbers (ASNs) 2015-AWP-288-NRA through 2015-AWP-292-NRA. At the time of writing of this staff report, the submittal been accepted by FAA for review and in a "proposed" status with no determination issued.

Open Area: Compatibility Zone D requires that 10% of area within major projects (10 acres or

larger) be set aside as open land that could potentially serve as emergency landing areas. However, this project site is less than 10 acres in size; therefore, open area requirements are not applicable.

**CONDITIONS:**

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) noise sensitive outdoor nonresidential uses, and hazards to flight.
3. The attached notice shall be given to all prospective purchasers and/or tenants of the property.
4. Any proposed detention basin(s) on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the retention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the retention basin(s) shall not include trees that

produce seeds, fruits, or berries.

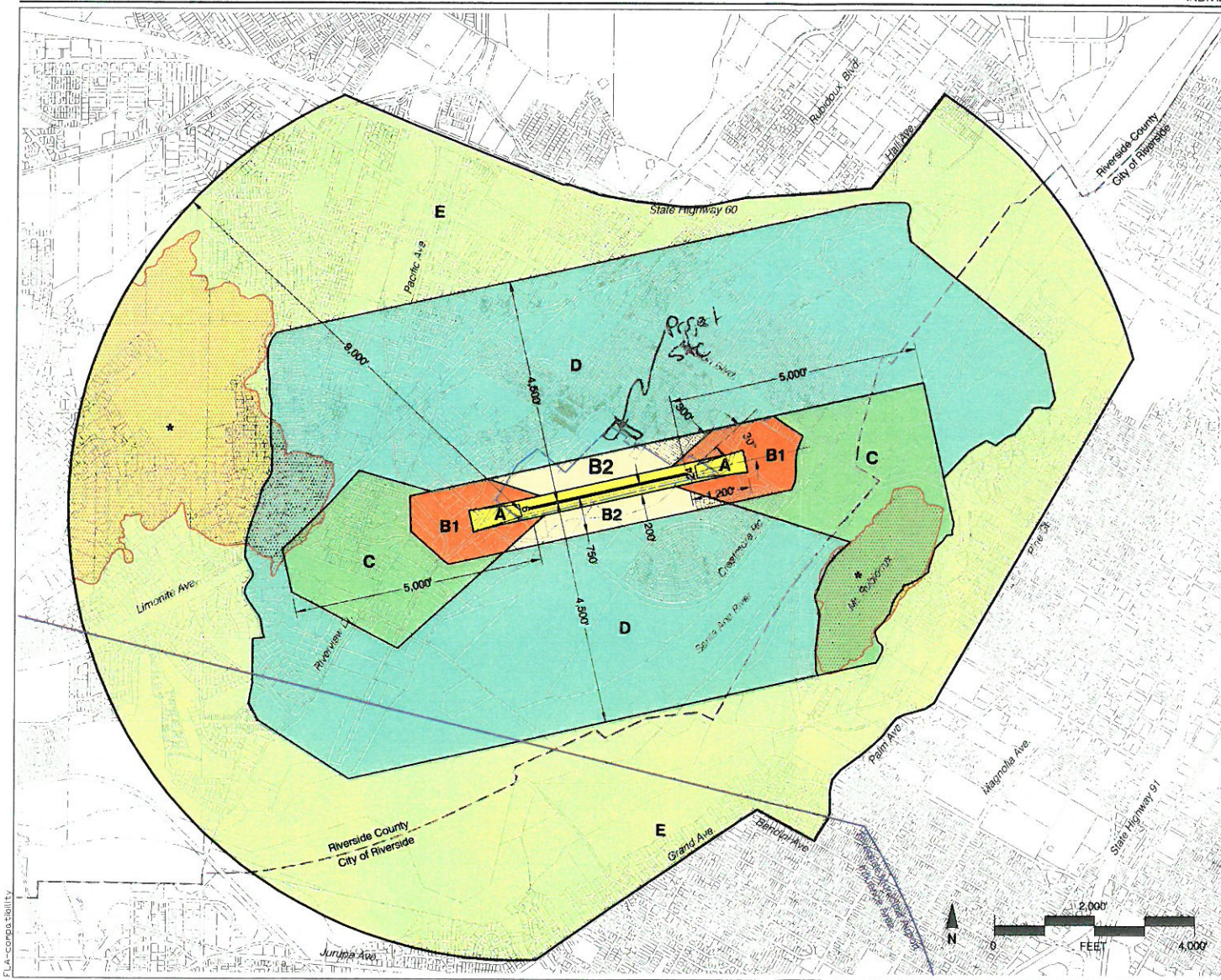
5. This project has been evaluated as a proposal for the establishment of a school with ancillary office use. The City of Jurupa Valley shall require additional review by the Airport Land Use Commission prior to the establishment of any of the following uses in this structure:

Commercial/service uses; civic uses; churches, chapels, and other places of worship; classrooms; day care centers; gymnasiums; theaters; conference or convention halls; auditoriums; fraternal lodges; bowling alleys; gaming; auction rooms; and office uses exceeding 7,500 square feet.

6. The proposed school shall be limited to a maximum enrollment of 250 students and a maximum staff of 15 to comply with Compatibility Zone D average and single-acre criteria.

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



**Legend**

**Compatibility Zones**

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E
- Height Review Overlay Zone

**Boundary Lines**

- Airport Property Line
- City Limits

**Note**

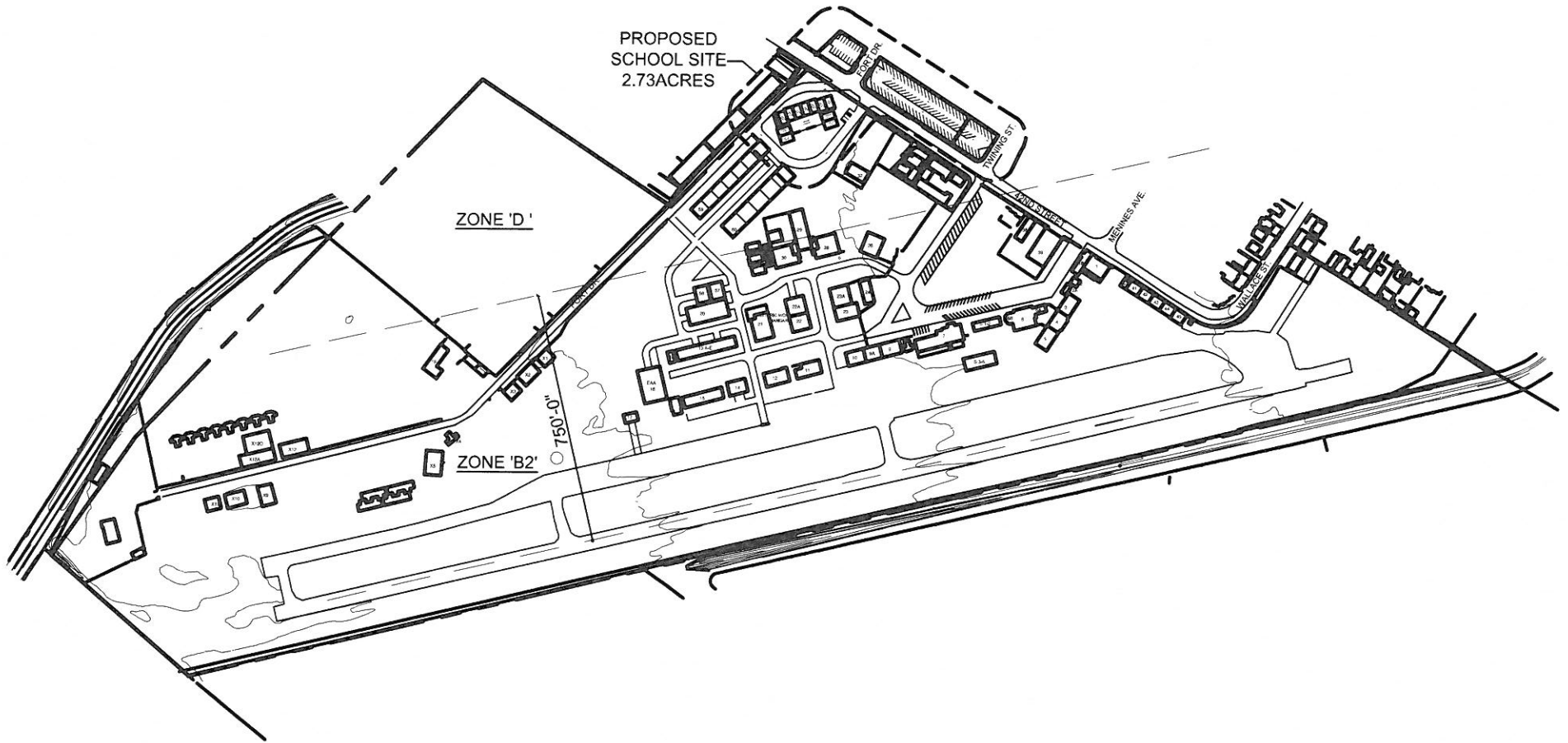
Airport influence boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

See Chapter 2, Table 2A for compatibility criteria associated with this map.

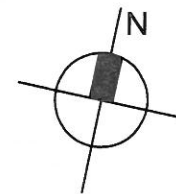
Riverside County  
 Airport Land Use Commission  
**Riverside County  
 Airport Land Use Compatibility Plan  
 Policy Document**  
 (Adopted December 2004)

Map FL-1

**Compatibility Map**  
 Flabob Airport



**ALUC RESUBMITTAL:**  
**PROPOSED SCHOOL SITE IN RELATION TO AIRPORT**  
**FLABOB AVIATION PREPARATORY ACADEMY**  
**JURUPA VALLEY, CALIFORNIA**

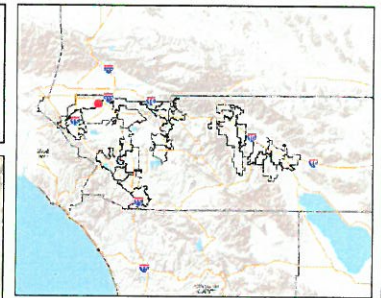


JOB# 820-02-14  
 DATE: 02-26-15



951 652-4431  
 951 652-0373 Fax  
 530 Saint John Place  
 Hemet, California 92543

# My Map



## Legend

- RCLIS Parcels
- City Boundaries
- Cities
- roadsanno
- highways
  - HWY
  - INTERCHANGE
  - INTERSTATE
  - OFFRAMP
  - ONRAMP
  - USHWY
- counties
- cities
- hydrographylines
- waterbodies
  - Lakes
  - Rivers



**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

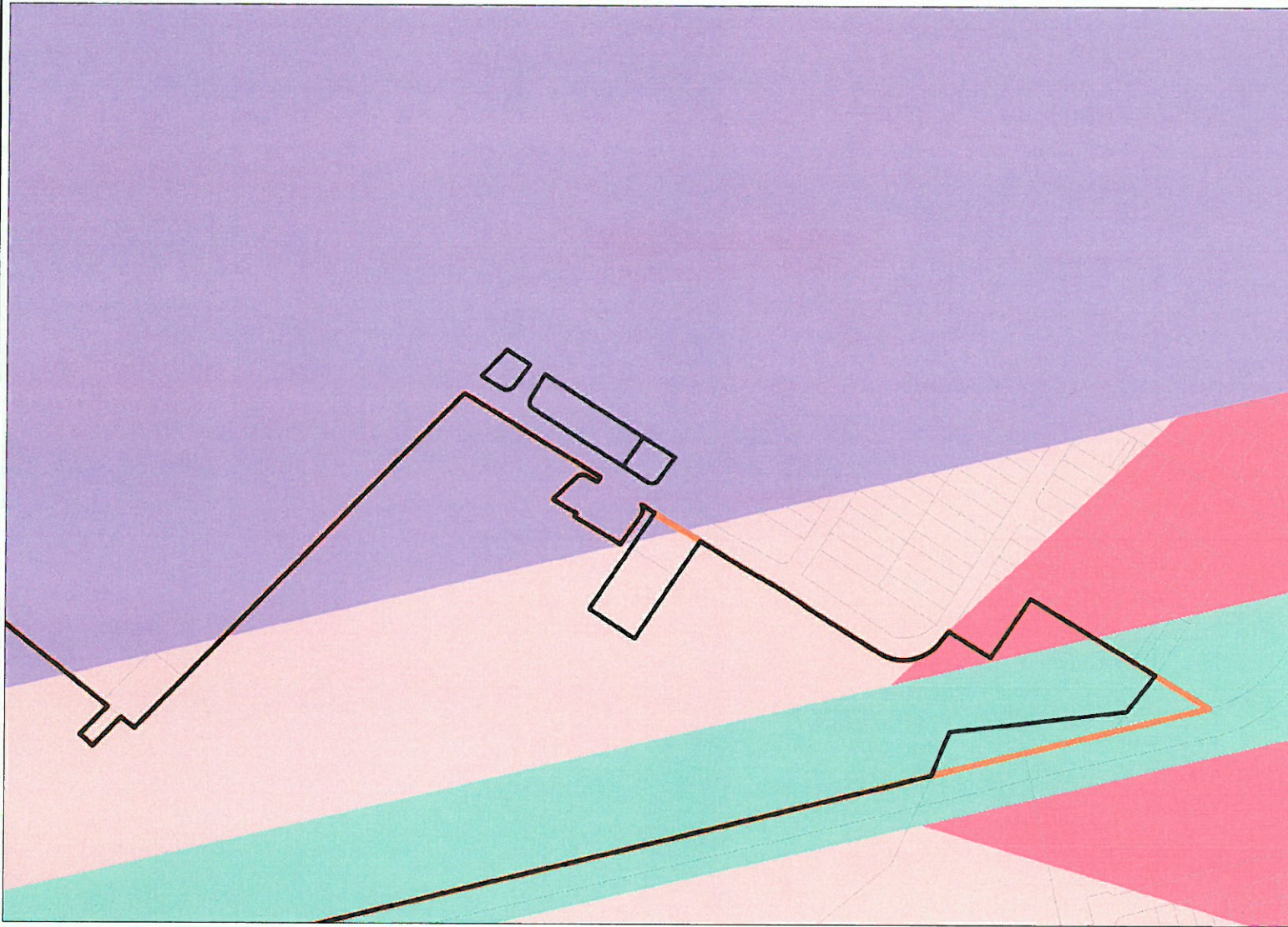
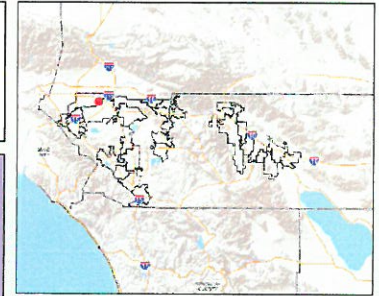
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## Notes

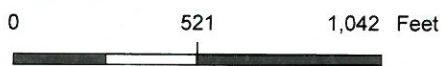


# My Map



### Legend

- RCLIS Parcels
- Airports
- AIA
- Airport Compatibility**
- <all other values>
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E
- City Boundaries
- Cities**
- roadsanno**
- highways**
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
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- counties
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- hydrographylines**
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- Lakes
- Rivers



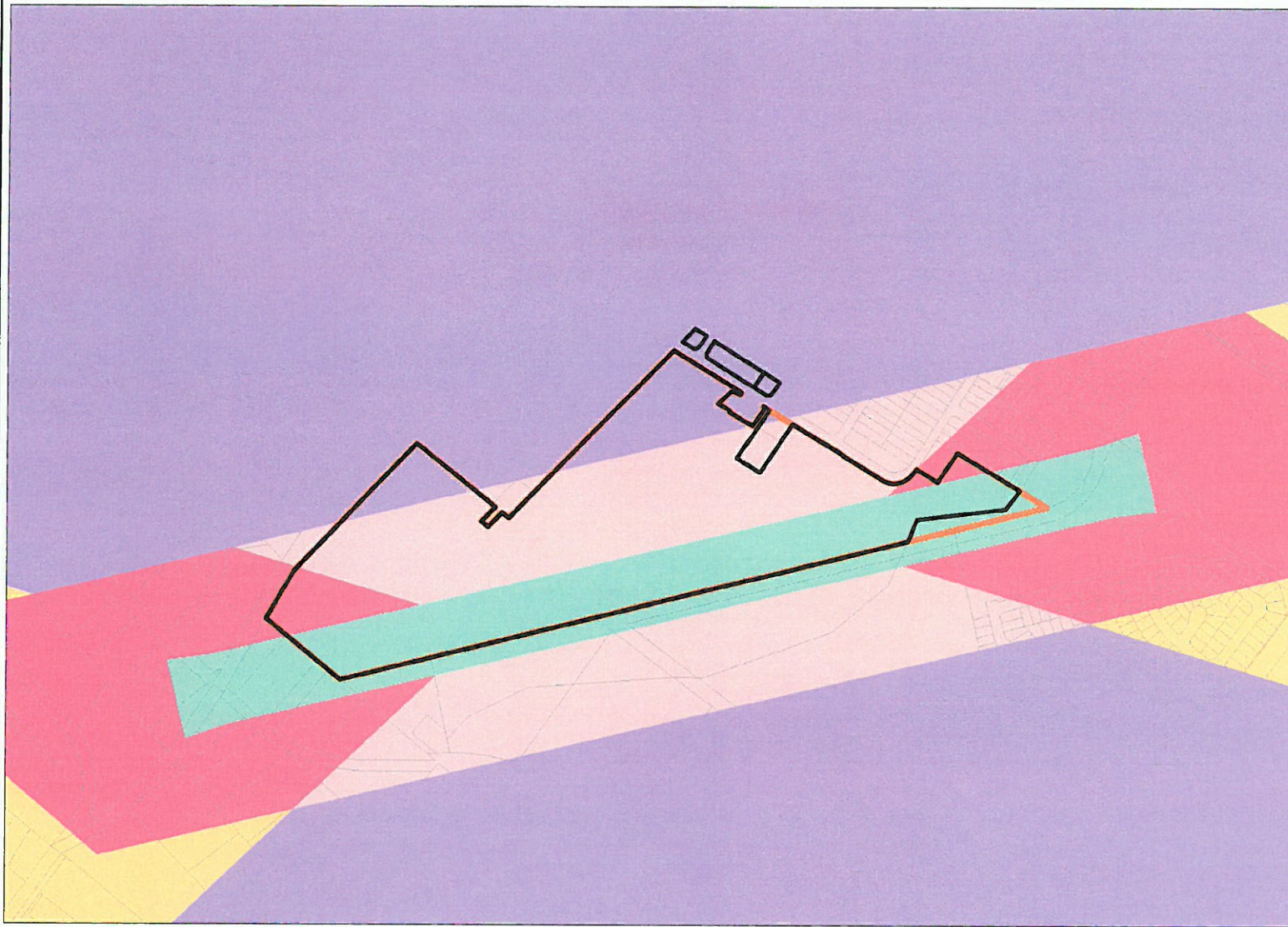
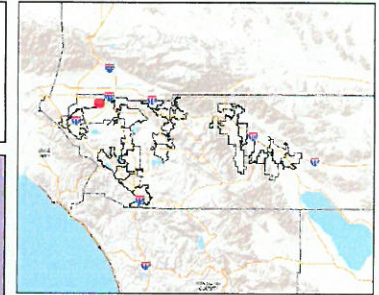
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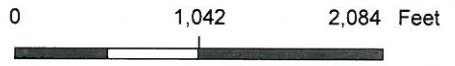
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### Notes

# My Map



- Legend**
- RCLIS Parcels
  - Airports
  - AIA
  - Airport Compatibility**
  - <all other values>
  - Zone A
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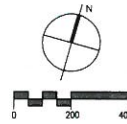
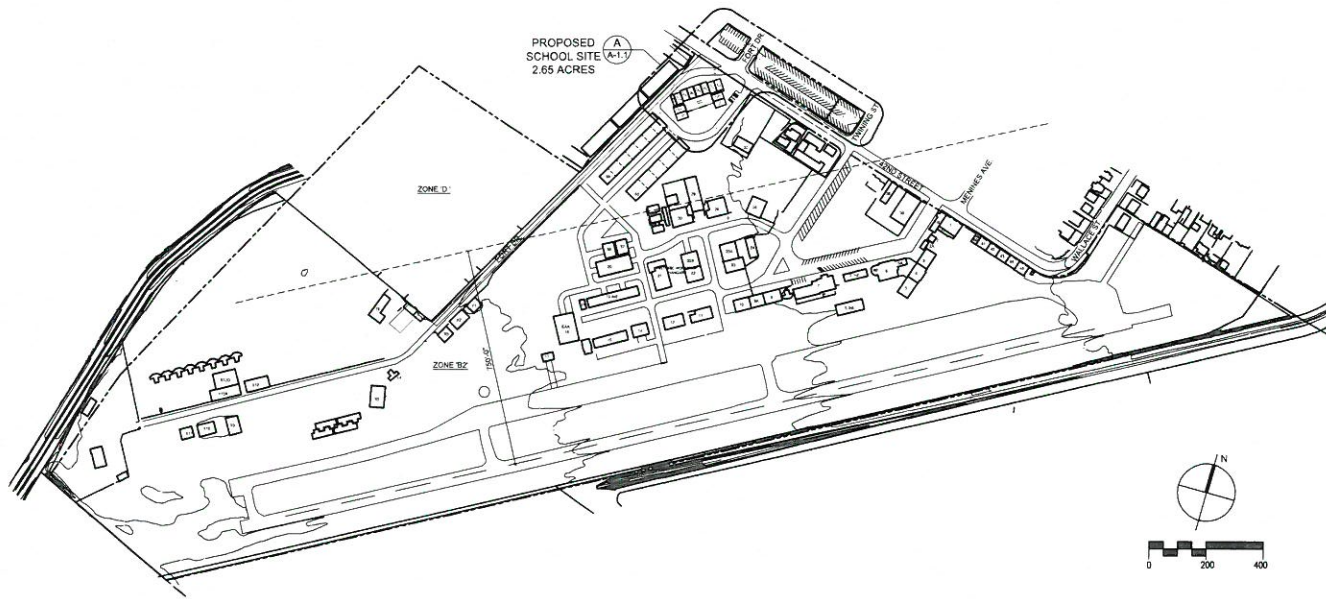
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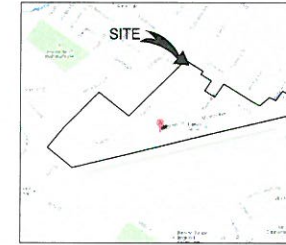
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**Notes**

# FLABOB AVIATION PREPARATORY ACADEMY AMENDED PUBLIC USE PERMIT



**VICINITY MAP:**



**SHEET INDEX:**

- 1 A-1.0 TITLE SHEET, SITE PLANS
- 2 A-1.1 ENLARGED SITE PLAN, FLOOR PLAN
- 3 A-2.0 FLOOR PLAN & EXTERIOR ELEVATIONS

**SITE DATA:**

ASSESSOR'S PARCEL NUMBERS: 181-190-010,  
181-151-001, -002, -003

SITE ADDRESS: 42ND STREET  
JURUPA VALLEY, CA 92509

TYPE OF USE: CHARTER SCHOOL

OCCUPANCY GROUP: E / B

NUMBER OF STORIES: ONE

ZONING DESIGNATION: MH (MANUFACTURING - HEAVY)

GENERAL LAND USE: PF (PUBLIC FACILITIES)

SITE AREA: 2.73 ACRES

PROPOSED BUILDING AREA: 15,848 S.F.

**LEGAL DESCRIPTION:**

RECORDED BOOK/PAGE: MB 1052, SUBDIVISION NAME: EVANS RIO  
RANCHO, LOT/PARCEL: 5

**PROJECT DESCRIPTION:**

AMENDED PUBLIC USE PERMIT APPLICATION FOR EXISTING FLABOB AVIATION PREPARATORY ACADEMY WITH 10 CLASSROOMS.

**PARKING REQUIRED:**

9' x 18' STANDARD  
11' x 18' END SPACE

1 SPACE PER CLASSROOM (ELEMENTARY & SECONDARY)  
1 SPACE PER 8 STUDENTS (HIGH SCHOOL)  
1 SPACE PER 200 S.F. ADMINISTRATION

**FLABOB SITE:**

10 CLASSROOMS  
4 HIGH SCHOOL (80 STUDENTS EA.) 16 SPACES  
6 ELEMENTARY / SECONDARY 8 SPACES  
400 S.F. OFFICE / ADMIN. 2 SPACES  
24 SPACES REQUIRED

**PROJECT DIRECTORY**

**APPLICANT:**  
RIVER SPRINGS CHARTER SCHOOL  
4130 MENEMES AVENUE  
JURUPA VALLEY, CA 92509  
(951) 222-4466

**ARCHITECT:**  
HERRON RUMANSOFF ARCHITECTS, INC.  
530 SAINT JOHN PLACE  
HEMET, CA 92343  
(951) 852-4431 PHONE  
(951) 852-0373 FAX  
CONTACT: RUSSELL RUMANSOFF

FLABOB AIRPORT - OVERALL SITE PLAN

SCALE  
1"=200'-0"

A

ALL NOTES, SPECIFICATIONS, MATERIALS, AND DIMENSIONS SHOWN ON THESE DRAWINGS ARE THE PROPERTY OF HERRON RUMANSOFF ARCHITECTS, INC. AND WILL BE THE PROPERTY OF HERRON RUMANSOFF ARCHITECTS, INC. ANY REVISIONS TO THESE DRAWINGS MUST BE APPROVED BY HERRON RUMANSOFF ARCHITECTS, INC.

HERRON RUMANSOFF ARCHITECTS, INC.  
 530 SAINT JOHN PLACE  
 HEMET, CA 92343  
 (951) 852-4431 PHONE  
 (951) 852-0373 FAX  
 CONTACT: RUSSELL RUMANSOFF

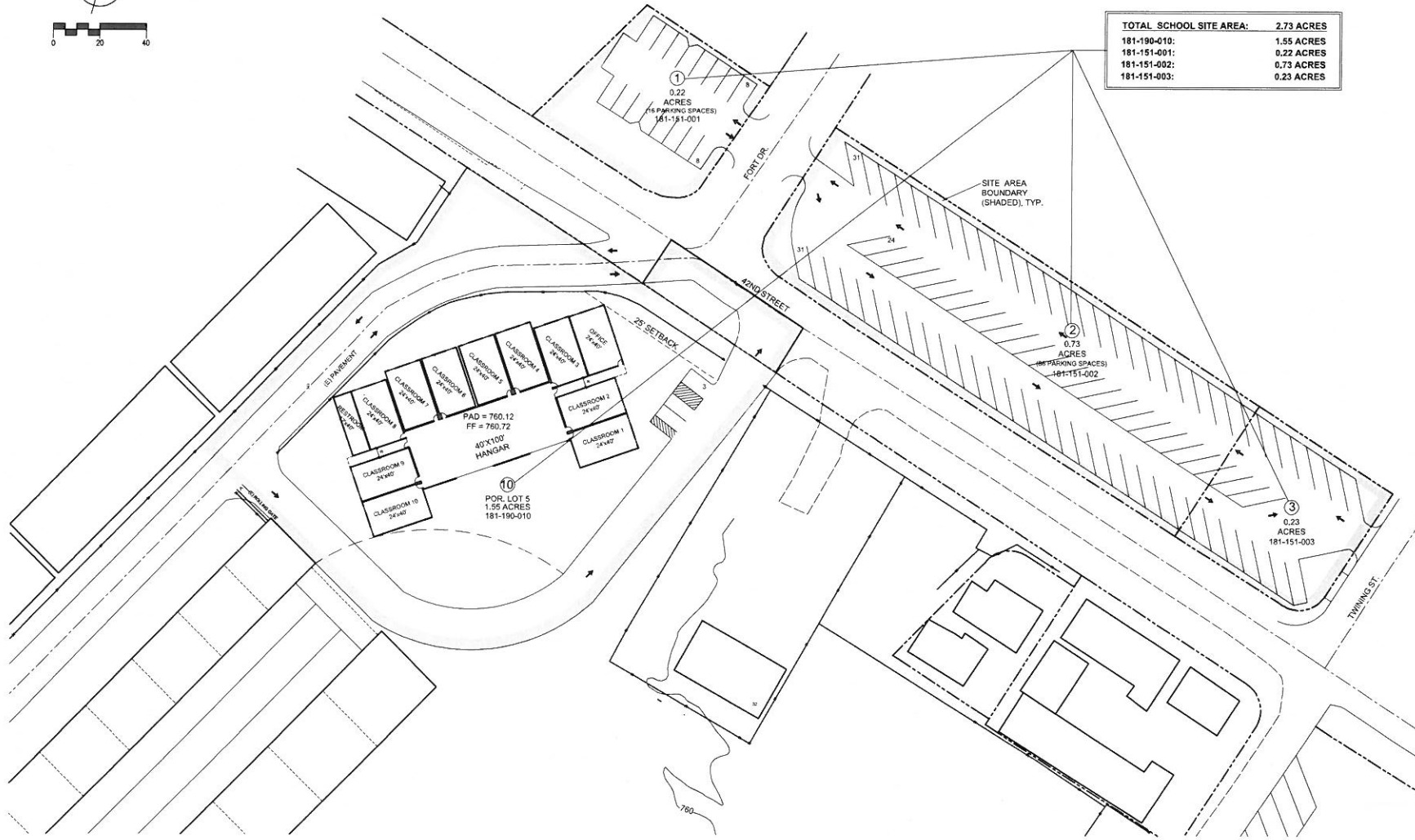
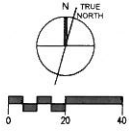
AMENDED PUBLIC USE PERMIT FOR:  
**FLABOB AVIATION PREPARATORY ACADEMY**  
 JURUPA VALLEY, CA 92509

PROJECT NO. 829-02-14  
 JOB NO. 029-02-14  
 SHEET NO. A-1.0  
 TITLE SHEET, SITE PLANS  
 DATE: 02/14/2014  
 REVISIONS: 02/14/2014  
 REVISED SITE AREA: 02/14/2014

ARCHITECT'S SIGNATURE: [Signature]

CONSULTANT: [Signature]

530 St. John Place Hemet, California 92343  
 951 852-4431 951 852-0373 Fax



<b>TOTAL SCHOOL SITE AREA: 2.73 ACRES</b>	
181-190-010:	1.55 ACRES
181-151-001:	0.22 ACRES
181-151-002:	0.73 ACRES
181-151-003:	0.23 ACRES

ENLARGED SITE PLAN

SCALE  
1" = 30'-0"

ALIC RESUBMITTAL 02/26/15  
REVISED SITE AREA 02/26/15

12/23/14  
ALIC REVIEW  
Issue purpose  
done

consultant  
architect's signature

AMENDED PUBLIC USE PERMIT FOR:  
**FLABOB AVIATION  
PREPARATORY ACADEMY**  
JURUPA VALLEY, CA 92509

Project

530 St. John Place Hemet, California 92343

architect, inc.

ENLARGED  
SITE PLAN

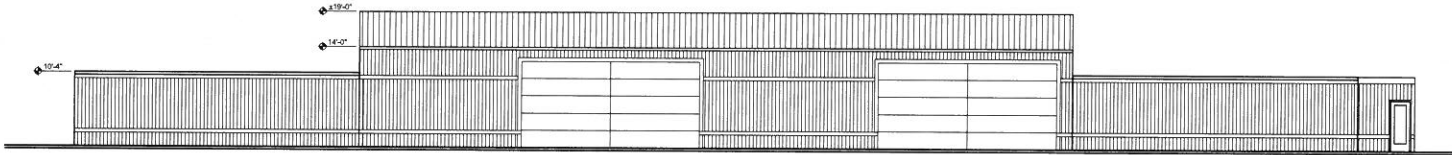
820-02-14

DATE: 02/26/15

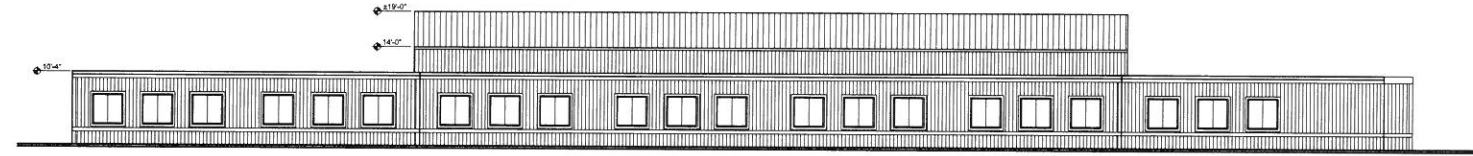
A-1.1

Sheet No.

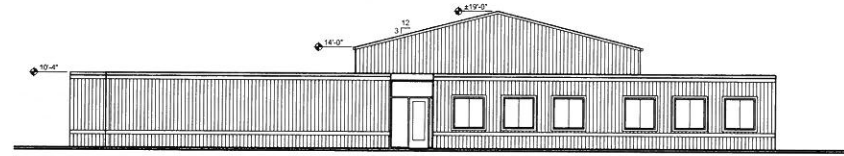
DATE DESCRIPTION



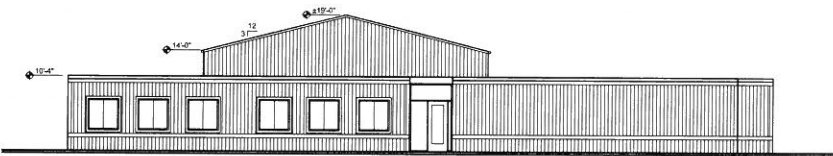
NORTH ELEVATION



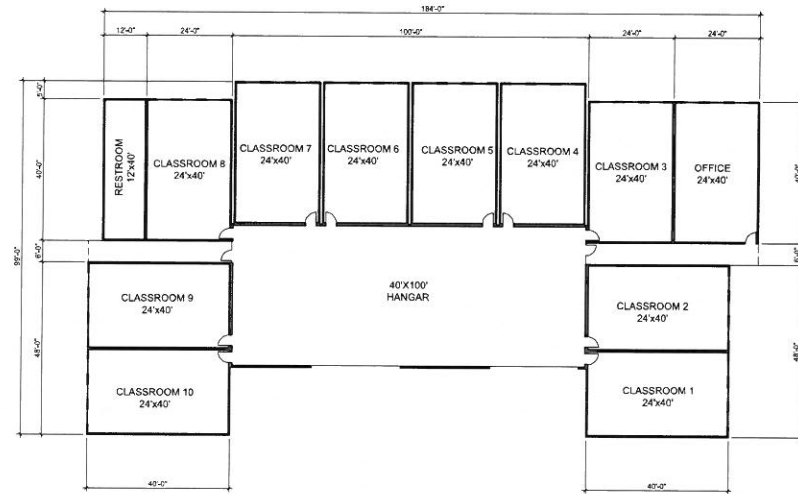
SOUTH ELEVATION



WEST ELEVATION



EAST ELEVATION



EXTERIOR ELEVATIONS

SCALE  
1/8" = 1'-0"

B

FLOOR PLAN - PROPOSED SCHOOL 15,648 S.F.

SCALE  
1/16" = 1'-0"

A

AMENDED PUBLIC USE PERMIT FOR:  
**FLABOB AVIATION  
PREPARATORY ACADEMY**  
JURUPA VALLEY, CA 92509

ALUC REVIEW 192314  
issue purpose date  
architect's signature  
consultant

ALUC RESUBMITAL 0225-15  
revision

**harrison  
rumanoff**  
architects, inc.

FLOOR PLAN,  
EXTERIOR  
ELEVATIONS  
820-02-14  
A-2.0

project 530 St. John Place Herms, California 92543 951 852-4431 951 852-0373 Fax

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3/2/15

Dear Russell,

The new school facilities for Flabob Airport Preparatory Academy (FAPA) are located in the D Zone of Flabob. In this zone, "children's schools" (K-12) are a "discouraged use." "Discouraged uses should generally not be permitted unless no feasible alternative is available."

**What is the reason that "no feasible alternative is available?"**

In summary, FAPA is an aviation-themed secondary school that wouldn't be relevant without the proximity and interaction of the Flabob Airport and the professionals who use it. In the words of the host Tom Wathen Center's mission statement, FAPA "uses the fascination of flight to inspire the love of learning for successful careers and satisfying lives . . . . At our historic Flabob Airport, there is excitement in working with the head, hand and heart, side by side with pilots, craftsmen and innovators in an authentic workplace." We believe that the embedding of the school in an authentic workplace, the opportunity for many and varied enrichment studies and projects, the chance for hands-on learning, and the sheer fascination of aviation, result in highly desirable student outcomes.

While many secondary schools have the goal of bringing the community into the learning, this is relatively difficult to do in practice. Community volunteers fall away without proximity. Students gain a great deal by being able to easily participate in the community of the airport, including programs such as Young Eagles. This participation is facilitated by our location.

**We are not alone in the belief that aviation-themed high schools are a powerful motivator.**

Some others include Raisbeck Aviation High School, which is just across the street from busy King County International Boeing Field near Seattle, and which is a little over 1,100 feet from the centerline of the runway. Given the class of airport, it appears that it would be within the B2 zone under RCALUC principles. It is ranked 7<sup>th</sup> best high school in the State of Washington, and as an aviation-themed college preparatory school specializes in Science, Technology, Engineering and Math (STEM).

The Newport News Aviation Academy is 2,800 feet from the runway centerline at Newport News/Williamsburg International Airport, a commercial (airline) airport in Newport News, Virginia.

At this highly regarded school, students have talked live to the astronauts aboard the International Space Station (as have students at FAPA), are building an airplane (as are FAPA students under a program of the Tom Wathen Academy), and are motivated to learn by aviation interest.

Altogether, according to a survey made by FAA in 2011, there are 69 aviation magnet schools in the United States. FAA has spoken in support of aviation magnet schools: "The FAA is engaged in a comprehensive program to modernize the Nation's air transportation system to meet the challenge of aviation growth in the coming decades. The modernization program takes advantage of current technological advances to increase the capacity of the Nation's air transportation system while reducing relative costs to the Nation's taxpayers. The FAA recognizes the increasing complexity of technical and managerial skills that will be needed to accommodate the technological advances in systems being planned and implemented throughout the aviation industry. FAA further recognizes that our educational system will play a critical role in preparing persons for careers in this advanced technological environment. For these reasons, FAA supports the development of aviation magnet secondary schools."

### **Aviation Programs Work!**

Research has confirmed the efficacy of "learning through aviation." See, for example, *What Evidence Exists to Verify That Learning through Aviation Works?* Journal of Aviation/Aerospace Education and Research, online at <http://commons.erau.edu/cgi/viewcontent.cgi?article=1109&context=jaer>.

Flabob Airport Preparatory Academy, and its direct predecessors, have successfully operated at Flabob Airport since the Fall semester of 2005, under a lease which continues in effect. During this time, FAPA has graduated 142 students. It has been the experience of FAPA that the students at FAPA are high performing. In 2014, FAPA graduated 14 students, 9 of whom were accepted to and enrolled in four year universities, including UCSD, UCR and UCM. The remaining 5 students went on to community college or military service. River Springs Charter School, operator of FAPA, had 14 National Merit Scholars among its 233 high school graduates in 2014; 12 of these were from FAPA. Our first robotics team, Robokong, was started at FAPA in 2008. They were the top seed rookie team their first year, the team spawned two other teams and is still going strong as an independent team. All 11th and 12th grade students participate in internships 6-8 hours per week, many of which occur at the airport. Not only do students get an opportunity to "try out" a potential job and add to their resume, but last year 28% of our interns received paying job offers at the end of their internship.

We know that FAPA's aviation theme and its location on Flabob Airport are literally life-changing for some students. For example, one student came to FAPA from other schools with the fixed intention to drop out of school as soon as the law allowed. She had certain learning difficulties and simply did not see how education was relevant to her. At FAPA, she became fascinated with

aviation, and worked on an airplane restoration project sponsored by the Tom Wathen Academy. She realized that she could not achieve any of her new aviation goals unless she applied herself to her studies, and at the same time she found new relevance in them because of the aviation connection. She has subsequently graduated from FAPA, attended San Bernardino Valley College where she obtained her Airframe and Powerplant Certificate (the "mechanic's license" required by FAA for work on airplanes), and is now employed full-time in the aviation industry. She has obtained her Private Pilot license. She is married to an Air Force non-commissioned officer, also a mechanic, and they are expecting their first child."

TWC and FAPA share the belief that the location of Flabob Airport Preparatory Academy on Flabob Airport, a working community of pilots, mechanics, teachers, restorers and builders, and persons whose lives have been touched by aviation, provides a rich and diverse community for implementation of shared goals. Real life learning can take place in the context of aviation, which necessarily embodies personal responsibility -- a pilot is responsible, often solely responsible, for his or her own life and that of others, and a mechanic is responsible for the safety and correctness of his or her work -- and also for cooperation with others -- aviation depends on mutual contributions of pilots, mechanics, designers, air traffic workers, and many others. In addition to classroom learning, the FAPA student will be able to work with pilots, craftsmen and innovators in the authentic workplace of the airport, all of which serves as a key resource for student real-world learning experience. Project-based learning and internships both on and off the airport will teach the hand as well as the heart. The combination of personal responsibility and mutual cooperation which characterizes aviation, the underlying theme of the school, will teach by example the principles of civic responsibility and participation which are essential to a citizen of a 21st century democracy.

The goal of FAPA is to prepare young people for success as lifelong learners, informed and active citizens, and community leaders by providing them with an innovative, academically rigorous, project-based education that connects students to community-based internships while being part of a safe and inclusive educational setting. The location of FAPA on a working, historic airport provides the opportunity to use the fascination of flight to inspire, to motivate, and to be subject matter of learning.

### **How Does Aviation Prepare Students for College and Career?**

Aviation is so broad in its reach that it can be connected to and illuminate study in almost any area of human learning. The following illustrates the hundreds of learning activities which are connected to aviation:



## AGRICULTURE

Aerial photography  
Agricultural aviation  
Australia's aviation  
Crop dusting  
Cloud seeding  
Economic implications  
Food and nutrition  
Infrared radiation  
International Agricultural  
Aviation Center  
International Flying  
Farmers  
Photosynthesis  
Weather  
Weather satellites  
ART  
Balloons13  
Commemorative stamps  
and  
Medals  
Da Vinci, Leonardo  
History of aviation  
Insignia  
Interiors of aircraft  
Kites  
Medals and decorations  
Model aircraft  
Mythology  
Objects of art  
Photography  
Pilot and crew wings  
Science fiction  
Trophies and awards  
ASTRONOMY  
Asteroids  
Astronautics  
Astronomy  
Astrophysics  
Celestial mechanics  
Celestial sphere  
Comets  
Constellations  
Cosmic rays  
Eclipse  
Galaxies  
International Years of the  
Quiet

Sun  
Interplanetary travel  
Kepler's laws  
Light  
Mariner probes  
Meteors  
Moon  
Observatories  
Orbiting observatories  
Orbits and trajectories  
Planetariums  
Planets  
Quantum theory  
Quasar  
Radio astronomy  
Relativity theory  
Solar system  
Stars  
Sun  
Telescopes  
Ultraviolet  
Universe  
X-rays  
BIOLOGY  
Animals in space  
Aviation medicine  
Biosatellites  
Bird flight  
Circadian rhythm  
Closed ecological system  
Extraterrestrial life  
Hydroponics  
Kosmos satellites  
Photosynthesis  
Space biology  
BUSINESS LAW  
Airports  
Certification procedures  
Crash investigation  
Government contracts  
Insurance  
Legal implications  
Patents  
Police and fire services  
Registration of aircraft  
CAREER GUIDANCE  
Air traffic control  
Army aviation

Astronauts  
Careers  
Charter flying  
Cryogenics  
Crystallography  
Cybernetics  
Flight instruction  
General aviation  
Government in aerospace  
Ground service and  
maintenance  
Manufacturing  
Occupations  
Pilots and pilot certificates  
Pilot training  
Spacecraft design  
Stewards and stewardesses  
Test pilots  
Women in aerospace  
CHEMISTRY  
Air  
Alloys  
Atoms  
Atmosphere  
Chemical energy  
Closed ecological system  
Cryogenics  
Elements  
Fuels  
Gases  
Lubricants  
Propellants  
Specific gravity  
EARTH SCIENCE  
Air masses  
Applications technology  
Satellites  
Astrogeology  
Astronautics  
Astronomy  
Astrophysics  
Atmosphere  
Aurora  
Aviation weather  
Boyle's law  
Charts  
Compasses  
Density altitude

Discoverer program  
Earth  
Environmental research  
satellites  
Explorer satellites  
Geodetic satellites  
Gravity  
Greenhouse effect  
Kosmos satellites  
Latitude and longitude  
Lightning  
Lunar charts  
Magnetic course  
Maps and mapping  
Mariner probes  
Meteorology  
Navigation systems  
Navigation techniques  
Oceanographic research  
Orbiting observatories  
Pilotage  
Precipitation  
Ranger  
Sounding rockets  
Surveyor  
Van Allen belts  
Weather  
Weather maps and  
charts<sup>14</sup>  
Weather satellites  
ECONOMICS  
Aerospace industry  
Airports  
Bush flying  
Business aviation  
Cargo aircraft  
Commercial airlines  
Commercial air transports  
Crop dusting  
Economic implications  
Fixed base operator  
Flight simulators  
General aviation  
Government contracts  
Government in aerospace  
Jet aircraft  
Jumbo jets  
Manufacturing

Production techniques  
Program management  
Supersonic transports  
Utility aviation  
GENERAL SCIENCE  
Airplane  
Astronomy  
Atmosphere  
Atoms  
Barometric pressure  
Bernoulli's principle  
Bird flight  
Clouds  
Electricity  
Energy  
Engines  
Fog  
Galaxies  
Helicopters  
Jet aircraft  
Launch Vehicles  
Man in flight  
Matter  
Mercury program  
Photography  
Planets  
Radio communications  
Satellites  
Saturn rockets  
Space stations  
Stars  
Sun  
Walk in space  
Weather  
Weather satellites  
GEOGRAPHY  
Bush flying  
Cartography  
Charts  
Compasses  
Course plotting  
European aerospace  
activities  
Latitude and longitude  
Magnetic course  
Maps and mapping  
Photography  
Photogrammetry

U.S.S.R. aerospace activities  
GEOLOGY  
Astrogeology  
Geodetic satellites  
Mountain, desert, and  
jungle  
Flying  
Photogrammetry  
Ranger  
Surveyor  
GOVERNMENT  
Aerospace industry  
Air Commerce Act  
Air traffic control  
Apollo  
Army aviation  
Coast Guard aviation  
Crash investigation  
FAA  
Federal Aviation  
Regulations  
Flight service station  
Government contracts  
Instrument Flight Rules  
Marine Corps aviation  
Mercury program  
Military aviation  
Military space program  
NASA  
National Airspace System  
National Transportation  
Safety  
Board  
Naval aviation  
Pilots and pilot certificates  
Registration of aircraft  
Visual Flight Rules  
HEALTH  
Aerospace medicine  
Animals in space  
Astronauts  
Circadian rhythm  
Drug effects  
Environmental control  
systems  
Flight physical  
Food and nutrition  
Human engineering

Hypoxia  
Life-support systems  
Man in flight  
Manned spaceflight  
Man-powered flight  
Pressurization  
Sensory deprivation  
Spacesuits  
Temperature control  
Weightlessness  
HISTORY  
Ace  
Air Commerce Act  
Air raid  
Altitude records  
Autogiros  
Balloons  
Barnstormers  
Battle of Britain  
Biographies  
Bomber aircraft  
Bush flying  
Commemorative stamps  
and  
Medals  
Desert Storm  
Dirigibles  
Distance records  
Endurance records  
First World War aircraft  
Flying circus  
Gliders  
History of aviation  
Korean War  
Luftwaffe  
Man-powered flight  
Mythology  
National Advisory  
Committee for  
Aeronautics15  
Rheims Air Meet  
Science fiction  
Second World War aircraft  
Speed records  
Vietnam War  
Women in aerospace  
World War I  
World War II

HOME ECONOMICS  
Fabrics  
Food and nutrition  
Interiors of aircraft  
Spacesuits  
Stewards and stewardesses  
INDUSTRIAL ARTS  
Aerial photography  
Aircraft propulsion systems  
Avionics  
Electronics  
General aviation aircraft  
Generators and alternators  
Interiors of aircraft  
Manufacturing  
Materials  
Metals and metallurgy  
Occupations preventive  
Maintenance  
Production techniques  
Refueling  
Spacecraft design  
INTERNATIONAL  
RELATIONS  
Air defense systems  
Air forces of the world  
Berlin airlift  
Commercial airlines  
DEW line  
Federation Aeronautique  
Internationale  
Five Freedoms  
International agreements  
International Geophysical  
Year  
International projects  
Iran-Iraq War  
Israeli-Arab Conflict 1967  
Missiles  
Political implications  
Reconnaissance Space law  
Tracking systems and  
networks  
United Nations  
MATHEMATICS  
Binary numbers  
Celestial navigation  
Course plotting

Cybernetics  
Dead reckoning  
Doppler navigation  
Escape velocity  
Information systems  
Navigation techniques  
Orbits and trajectories  
Parabola  
Telemetry  
Weight and balance  
MEDICINE  
Acceleration  
Aerospace medicine  
Animals in space  
Astronauts  
Aviation medicine  
Circadian rhythm  
Closed ecological system  
Decompression  
Drug effects  
Environmental control  
systems  
Environmental simulators  
Escape systems  
Flight physical  
High-altitude flight training  
Human engineering  
Hypoxia  
Life-support systems  
Man in flight  
Manned spaceflight  
Mercury program  
Parachutes  
Pressurization  
Psychological factors of  
flight  
Re-entry vehicles  
Sensory deprivation  
Space biology  
Spaceflight training  
Space medicine  
Spacesuits technological  
projections  
Walk in space  
Weightlessness  
X-rays  
METEOROLOGY  
Air

Air masses  
Atmosphere  
Barometric pressure  
Clouds  
Convection currents  
Earth science  
Evaporation and condensation  
Fog  
Humidity  
Ozone  
Precipitation  
Turbulence  
Weather maps and charts  
Weather satellites  
Wind  
PHYSICS  
Acoustics  
Aerodynamics  
Aircraft propulsion systems  
Airfoil  
Airplane  
Airspeed indicator  
Alloys  
Area rule  
Astronautics  
Attitude control  
Automatic landing  
Astrionics  
Avionics  
Bank  
Bearing  
Bernoulli's principle  
Boyle's law  
Carburetion  
Center of gravity  
Computers  
Cryogenics  
Crystallography  
Doppler effect  
Dynamic soaring  
Electricity  
Electromagnetism  
Electronics  
Energy  
Engines  
Escape velocity  
Flight management

Fluid mechanics<sup>16</sup>  
Gas turbine engines  
Ground-effect machines  
Gyroscope heat energy  
Heat shields  
High-lift devices  
Hydraulic systems  
Hypersonic flight  
Inertial guidance  
Infrared radiation  
Instrument panel  
Lasers  
Launching  
Lifting-body vehicles  
Magnetic levitation (MAGLEV)  
Maneuvers  
Matter  
Measurement of power  
Metals and metallurgy  
Newton's laws  
Noise  
Nuclear energy  
Nuclear propulsion  
Pilot-static system  
Plasma  
Power management  
Radar  
Radiation  
Radio  
Reciprocating engines  
Rendezvous and docking  
Robots  
Rotating combustion engines  
Sailplanes  
Semiconductors  
Shock wave  
Solar cells  
Solid-state physics  
Space propulsion systems  
Supersonic flight  
Television  
Temperature scales  
V-STOL aircraft  
Wind tunnels  
Wings  
X-rays

PSYCHOLOGY  
Astronauts  
Aviation medicine  
Cosmonauts  
Flying safety  
Gemini  
Human factors  
Man in flight  
Pilot training  
Psychological factors of flight  
Spaceflight training  
Space medicine  
SOCIAL STUDIES  
Air defense systems  
Air forces of the world  
Airmail  
Air taxis  
Apollo  
Army aviation  
Atlas missile  
Berlin airlift  
Biographies  
Blockhouse  
Bombs  
Careers  
Cargo aircraft  
Commercial airlines  
Communications satellites  
Crop dusting  
Cybernetics  
Demonstration teams  
DEW line  
Economic implications  
Educational implications  
Eurosace  
European aerospace activities  
FAA  
Fighter aircraft  
Fixed base operation  
Flight (as passenger)  
Flight test programs  
Flying doctor services  
Forest fire control  
Gemini General aviation  
Gliders  
Gliding

Government in aerospace  
Hangars  
Helicopters  
Heliports  
High-speed surface transportation  
History of aviation  
Homebuilt aircraft  
Instrument flight techniques  
Insurance  
Interplanetary travel  
Israeli-Arab Conflict - 1967  
Jet aircraft  
Jumbo jets  
Kamikaze  
Kennedy Space Center  
Korean War  
Launch facilities  
Launch vehicles  
Luftwaffe  
Lunar bases  
Lunar exploration  
Manned Orbiting Laboratory  
Manned spaceflight  
Manufacturing  
Mercury program  
Military aircraft  
Military implications  
Military space program  
Missiles  
Mythology  
NASA  
Naval aviation  
NORAD  
Oceanographic research  
Peenemuende  
Polar flights  
Police and fire services  
Preflight training  
Production techniques  
Program management  
Radio communications  
Rescue and recovery service  
Rockets and rocketry  
Runways

Safety statistics  
Sailplanes  
Satellites  
Saturn rockets  
Search and rescue  
Social implications  
Space stations  
Sport flying  
Strategic Air Command  
Supersonic transports  
Systems engineering  
Technological projections  
Unidentified flying objects  
Utility aviation  
Weaponry  
Wind tunnels  
X-series aircraft  
SPEECH AND COMMUNICATIONS  
Air traffic control<sup>17</sup>  
Communications satellites  
Ground control approach  
Morse code  
Phonetic alphabet  
Terminology of aerospace



Federal Aviation  
Administration

<< OE/AAA

## Case Submission Success

Project Rive-244335952-15 has been submitted successfully to the FAA.

Your filing is assigned Aeronautical Study Number (ASN):

**2015-AWP-288-NRA**

**2015-AWP-289-NRA**

**2015-AWP-290-NRA**

**2015-AWP-291-NRA**

**2015-AWP-292-NRA**

Please refer to the assigned ASN on all future inquiries regarding this filing.

Please return to the system at a later date for status updates.

*To ensure e-mail notifications are delivered to your inbox please add [noreply@faa.gov](mailto:noreply@faa.gov) to your address book. Notifications sent from this address are system generated FAA e-mails and replies to this address will NOT be read or forwarded for review. Each system generated e-mail will contain specific FAA contact information in the text of the message.*

# NOTICE OF PUBLIC HEARING

## RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14<sup>th</sup> Floor, Riverside, California 92501, Monday through Thursday, from 8:00 a.m. to 5:00 p.m., and by prescheduled appointment on Fridays, from 9:00 a.m. to 5:00 p.m.

PLACE OF HEARING: Riverside County Administration Center  
4080 Lemon St., 1<sup>st</sup> Floor Hearing Room  
Riverside, California

DATE OF HEARING: April 9, 2015

TIME OF HEARING: 9:00 A.M.

### CASE DESCRIPTION:

ZAP1023FL14 – River Springs Charter School (Representative: Dave Black) – City of Jurupa Valley Major Action Case No. MA1492, consisting of Public Use Permit No. 1401. A proposal to relocate and establish Flabob Airport Preparatory Academy as a charter school, including the development of 17,690 square feet of building area (ten classrooms to serve elementary, secondary, and high school students and a 400 square foot administrative office), to be located in the northwesterly portion of the grounds of Flabob Airport, southerly of 42<sup>nd</sup> Street, easterly of Fort Drive and westerly of Twining Street. (Airport Compatibility Zone D of the Flabob Airport Influence Area).

FURTHER INFORMATION: Contact Russell Brady at (951) 955-0549 or John Guerin at (951) 955-0982. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Ms. Annette Tam of the City of Jurupa Valley Planning Department, at (951) 332-6464.

**APPLICATION FOR MAJOR LAND USE ACTION REVIEW**  
**RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**

ALUC Identification No.

ZAP1023FL14

**PROJECT PROPONENT (TO BE COMPLETED BY APPLICANT)**

Date of Application 12/22/14  
 Property Owner The Wathen Group Phone Number (951) 683-2309  
 Mailing Address 4130 Mennes Ave  
Jurupa valley, CA 92509 # 24

Agent (if any) Dave Black Phone Number 760 330 6176  
 Mailing Address 43174 Business Park Dr. # 101  
Temecula, CA. 92590

**PROJECT LOCATION (TO BE COMPLETED BY APPLICANT)**

*Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways*

Street Address 4130 Mennes A  
Riverside, CA 92509 #  
 Assessor's Parcel No. 181-190-010 Parcel Size \_\_\_\_\_  
 Subdivision Name \_\_\_\_\_ Zoning  
 Lot Number \_\_\_\_\_ Classification D

**PROJECT DESCRIPTION (TO BE COMPLETED BY APPLICANT)**

*If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed*

Existing Land Use (describe) Empty dirt area. No current use

Proposed Land Use (describe) Flabob airport Preparatory Academy site

For Residential Uses Number of Parcels or Units on Site (exclude secondary units) \_\_\_\_\_  
 For Other Land Uses Hours of Use \_\_\_\_\_  
 (See Appendix C) Number of People on Site \_\_\_\_\_ Maximum Number \_\_\_\_\_  
 Method of Calculation \_\_\_\_\_

Height Data Height above Ground or Tallest Object (including antennas and trees) \_\_\_\_\_ ft.  
 Highest Elevation (above sea level) of Any Object or Terrain on Site \_\_\_\_\_ ft.

Flight Hazards Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?  Yes  No  
 If yes, describe \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_



**REFERRING AGENCY** (APPLICANT OR JURISDICTION TO COMPLETE)

Date Received \_\_\_\_\_

Agency Name

City of Turupa

Staff Contact

Annette Tam

Phone Number

951-332-6464

Agency's Project No.

MA 1492 (PUP1401)

Type of Project

General Plan Amendment

Zoning Amendment or Variance

Subdivision Approval

Use Permit

Public Facility

Other \_\_\_\_\_

**A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

**B. SUBMISSION PACKAGE:**

**ALUC REVIEW**

- 1 . . . . . Completed Application Form
- 1 . . . . . Project Site Plan – Folded (8-1/2 x 14 max.)
- 1 . . . . . Elevations of Buildings - Folded
- 1 Each . 8 ½ x 11 reduced copy of the above
- 1 . . . . . 8 ½ x 11 reduced copy showing project in relationship to airport.
- 1 Set . Floor plans for non-residential projects
- 4 Sets. . Gummed address labels of the Owner and representative (**See Proponent**).
- 1 Set. . Gummed address labels of all property owners within a 300' radius of the project site. If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10), with ALUC return address.
- 4 Sets. . Gummed address labels of the referring agency (City or County).
- 1 . . . . . Check for Fee (See Item "C" below)

**STAFF REVIEW (Consult with ALUC staff planner as to whether project qualifies)**

- 1 . . . . . Completed Application Form
- 1 . . . . . Project Site Plans – Folded (8-1/2 x 14 max.)
- 1 . . . . . Elevations of Buildings - Folded
- 1 . . . . . 8 ½ x 11 Vicinity Map
- 1 Set . Gummed address labels of the Owner and representative (**See Proponent**).
- 1 Set . Gummed address labels of the referring agency.
- 1 . . . . . Check for review–See Below

**COUNTY OF RIVERSIDE  
AIRPORT LAND USE COMMISSION**

**STAFF REPORT**

**AGENDA ITEM:** 3.4

**HEARING DATE:** April 9, 2015

**CASE NUMBER:** ZAP1020CH15 – Wal-Mart Stores, Inc. (Representatives: Kathleen Bonesz, Kimley-Horn and Associates; Jonathan Shardlow, Gresham Savage Nolan and Tilden)

**APPROVING JURISDICTION:** City of Eastvale

**JURISDICTION CASE NO:** 12-0051 Major Development Plan/Conditional Use Permit, General Plan Amendment [GPA], Change of Zone [CZ], Tentative Tract Map No. 36051

**MAJOR ISSUES:** Project intensity, Open area, Site split by Zone C/Zone D boundary. The single-acre intensities within two of the acres on the westerly side of the project could potentially exceed allowable single-acre intensities in Compatibility Zone C. Staff recommends that this be addressed by limiting serving area within the fast food restaurant to 1,500 square feet and prohibiting restaurant uses in the retail buildings.

**RECOMMENDATION:** Staff recommends a finding of CONSISTENCY for the Change of Zone, General Plan Amendment, and Tentative Tract Map. Staff recommends that the Major Development Plan/Conditional Use Permit be found CONSISTENT with the 2008 Chino Airport Land Use Compatibility Plan, subject to the conditions included herein.

**PROJECT DESCRIPTION:** The applicant proposes to develop Eastvale Crossings, a 218,100 square foot shopping center featuring a 192,000 square foot Wal-Mart, on 24.8 gross acres. In addition to the Wal-Mart, the shopping center will include two retail buildings with a combined floor area of 18,400 square feet, a 3,500 square foot fast food restaurant with drive-thru, and a gas station with a 4,200 square foot convenience store, 16 fueling positions, and a car wash. The site is presently split between areas designated Light Industrial and Commercial Retail on the City's General Plan. The GPA would designate the entire site as Commercial Retail. Similarly, the site is presently split between areas zoned C-1/C-P (General Commercial) and M-SC (Manufacturing-Service Commercial). The CZ would apply C-1/C-P zoning to the entire site. Tentative Tract Map No. 35061 would divide the property into six (6) parcels.

**PROJECT LOCATION:** The project site is located at the southeast corner of Archibald Avenue and Limonite Avenue, northerly and westerly of Southern California Edison transmission lines and James C. Huber Park in the City of Eastvale, approximately 9,271 feet easterly of the easterly terminus of Runway 8R-26L at Chino Airport.

**LAND USE PLAN:** 2008 Chino Airport Land Use Compatibility Plan

- a. Airport Influence Area: Chino Airport
- b. Land Use Policy: Airport Compatibility Zones C and D
- c. Noise Levels: 55 CNEL or less

**BACKGROUND:**

Non-Residential Average Intensity: The site is located in Airport Compatibility Zones C and D. Nonresidential intensity in Airport Compatibility Zone C is restricted to an average of 75 persons per acre. Pursuant to Additional Compatibility Policy 2.6 of the Chino Airport Land Use Compatibility Plan [Chino ALUCP], average nonresidential intensity in Airport Compatibility Zone D may be up to 150 persons per acre. Pursuant to Additional Compatibility Policy 2.7 of the Chino ALUCP, the intensity of retail areas has been determined to be one person per 115 square feet of gross floor area. The “Building Code Method” for calculating intensity utilizes “minimum floor area per occupant” criteria from the Building Code as a factor in projecting intensity. Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, the intensity of restaurant serving area has been determined to be one person per 15 square feet and the intensity of commercial kitchen areas has been determined to be one person per 200 square feet. Vehicle occupancy would be estimated at 1.5 persons per vehicle in the restaurant drive-thru queue and at each of the fueling pumps. If one were to assume as a worst-case scenario that the entire fast food restaurant were serving area, the total occupancy of the site would be estimated at 2,120 people. This would result in an average intensity of 86 people per acre across the 24.6-acre site. However, if one were to include the adjacent Southern California Edison easement, the gross area of the site would increase to 34.3 acres, resulting in an average intensity of 62 persons per acre, which would be consistent with criteria for Airport Compatibility Zone C as well as Zone D.

An overall average intensity of 86 persons per acre exceeds allowable average intensities for Airport Compatibility Zone C. However, all uses other than the Wal-Mart would have a maximum occupancy of 450 persons together. The above numbers are based on a total of 1,670 persons in the Wal-Mart building. As discussed below in the discussion of single-acre intensity, we estimate the actual occupancy of the Wal-Mart based on the customer count conducted at a Wal-Mart in the City of Ontario, plus Wal-Mart employees/associates, at 747 persons. This would result in a total occupancy of 1,197 persons or less, and an average intensity of 49 persons per acre.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle in the absence of more precise data). Based on the 1,063 parking spaces provided and assuming an additional 10% use by public transportation, the total occupancy would be estimated at 1,754 persons or an average of 71 persons per acre across the 24.6-acre site.

The applicant provided an alternate occupancy analysis based on an existing Wal-Mart located at 1333 N. Mountain Avenue in Ontario. The survey of occupancy was conducted during nine two-

hour periods over a consecutive nine-day period in January, 2015. These included four late afternoons (4:00-6:00 PM) on Sunday, Tuesday, Thursday, and Saturday, three early afternoons (1:00-3:00 PM) on Saturday, Friday, and Sunday, and two weekday mornings (7:00-9:00 AM) on Monday and Wednesday. The highest customer count was reached at 2:20 PM on a Sunday afternoon: 582 persons. It should be noted that this count did not include employees. As a supplement, Wal-Mart has provided a statement that the maximum anticipated number of employees at the proposed site would be 165. On this basis, staff estimates the occupancy of the proposed Wal-Mart store at 747.

Unfortunately, the proposed Wal-Mart would be located in portions of two Compatibility Zones, and there is no way to determine how many of the people in the building would be within the portion in Zone C vis-a-vis the portion in Zone D.

Non-Residential Single-Acre Intensity: Nonresidential single-acre intensity is restricted to 150 persons in any given acre within Airport Compatibility Zone C. This level may be increased to up to 195 with use of risk-reduction design features, including, but not limited to, the following possible mitigation measures: limiting buildings to a single story; enhancing the fire sprinkler system; increasing the number of emergency exits; upgrading the strength of the building roof; avoiding skylights; limiting the number and size of windows; and using concrete walls. The project applicant has requested credit for each of these risk-reduction design measures for the Wal-Mart, other than absence of skylights.

As noted above, use of the retail occupancy standard applied in the Chino Airport Influence Area (one person per 115 square feet of gross floor area) leads to a determination that a 192,000 square foot store would accommodate 1,670 persons. Such a store occupies 4.41 acres, so the single-acre occupancy assuming an even distribution of customers and associates throughout the establishment would be 379 persons. This level would be consistent with compatibility criteria for Zone D, but would be inconsistent with compatibility criteria for Zone C. Discussion of this concern stimulated the need for the applicant team to authorize the above-referenced customer count. Using the 747 total occupancy, an even distribution throughout the store would result in a single-acre occupancy of 169 persons, which is 12.9% above the maximum single-acre occupancy for Zone C. However, staff has reviewed the proposed risk-reduction design measures and believes that a bonus of at least 20 percent is in order, which would allow a single-acre intensity of up to 180 persons in Zone C.

The two retail buildings and the 3,500 square foot fast food restaurant located in the westerly portion of the property along Archibald Avenue are also in Compatibility Zone C. The floor plans of these buildings are unknown, and so are any risk-reduction measures (although a small credit could be achieved if all are single-story structures). Square acres can be analyzed that include the entirety of the fast food restaurant, plus portions of one of the retail buildings. Specifically, an area including the northerly 3,510 feet of the larger (southerly) retail building would be included in a square acre that includes the fast food restaurant. Assuming one person per 115 square feet, this area would accommodate approximately 31 persons, leaving 119 for the restaurant with drive-thru. Assuming six vehicles in the restaurant drive-thru and an average occupancy of 1.5 persons per vehicle, nine

persons would be in the drive-thru, leaving 110 available for the restaurant. Limiting the restaurant serving area to 1,500 square feet (with 2,000 square feet for the commercial kitchen and other areas) would result in an estimated occupancy of 110, which would be consistent.

Given the current limitations on nonresidential intensity in Compatibility Zone C, our recommended conditions must include the above limitation on the restaurant and prohibition of additional restaurant uses within the retail buildings.

Noise: Under ultimate airport development conditions, the aircraft noise level at this location would be at or below an average of 55 dB(A) CNEL, with the contour touching the extreme northwesterly corner of the property. All buildings would be outside the area subject to average aircraft noise levels above 55 CNEL. Therefore, no special measures to mitigate aircraft-generated noise are required.

PART 77: The elevation of Runway 8R-26L at its easterly terminus is approximately 636.5 feet above mean sea level (AMSL). At an approximate distance of 9,271 feet from the runway, any structure above 729.2 feet AMSL at its top point would require FAA review. The project plans indicate a finished floor elevation of 644 feet AMSL and a building height of 32 feet for a maximum elevation at top point of 676 feet AMSL. Therefore, Federal Aviation Administration (FAA) obstruction evaluation review for height/elevation reasons is not required.

Open Area: Compatibility Zone C requires that 20% of area within major projects (10 acres or larger) be set aside as open land that could potentially serve as emergency landing areas, and Compatibility Zone D requires that 10% of the project area be set aside for this purpose.

Excluding the Southern California Edison easement and 1.52 acres to be dedicated as right-of-way, the project site includes 23.25 acres – 14.35 acres in Zone C and 8.9 acres in Zone D. As a result, the project must devote at least 2.87 acres in Zone C and 0.89 acres in Zone D to ALUC-qualifying open area. The project team has prepared an exhibit depicting 2.89 acres in Zone C and 0.98 acres in Zone D that would meet the definition of ALUC-qualifying open area.

**CONDITIONS:**

1. Any outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky.
2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.

- (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area, including landscaping utilizing water features, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, and incinerators.
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Children's schools, hospitals, nursing homes (skilled nursing facilities), day care centers (including children's nurseries), and libraries.
  - (f) Highly noise-sensitive outdoor nonresidential uses and hazards to flight.
3. This finding of consistency is based upon the site plan dated October 22, 2014. Any changes in the locations, heights, layout, or intended use of buildings, except as necessary to comply with one or more of the conditions herein, shall be subject to further review by the Airport Land Use Commission as an amended project.
4. The following occupancy and use area limits are applicable to the buildings shown on the site plan reviewed by the Airport Land Use Commission, dated October 22, 2014, in the absence of a subsequent review and determination as to consistency by ALUC or its staff.
- (a) The building labeled "fast food" shall be limited to a total gross floor area of 3,500 square feet, not more than 1,500 square feet of which shall be restaurant serving area.
  - (b) The buildings labeled as "Retail" shall not be used as restaurants.
5. The City of Eastvale shall either prohibit the following uses on this site, or shall require additional review by the Airport Land Use Commission prior to establishment of any of the following uses in any of the structures proposed through this Major Development Plan/ Conditional Use Permit:

Auction rooms, auditoriums, bowling alleys, churches and chapels, classrooms (adult), conference rooms exceeding 300 square feet in area, dance floors, drinking establishments, gaming, gymnasiums, lodge rooms, lounges, restaurants (except for one freestanding restaurant – serving area not to exceed 1,500 square feet – and one restaurant concession within the Wal-Mart building), reviewing stands, stages, skating rink and swimming pool

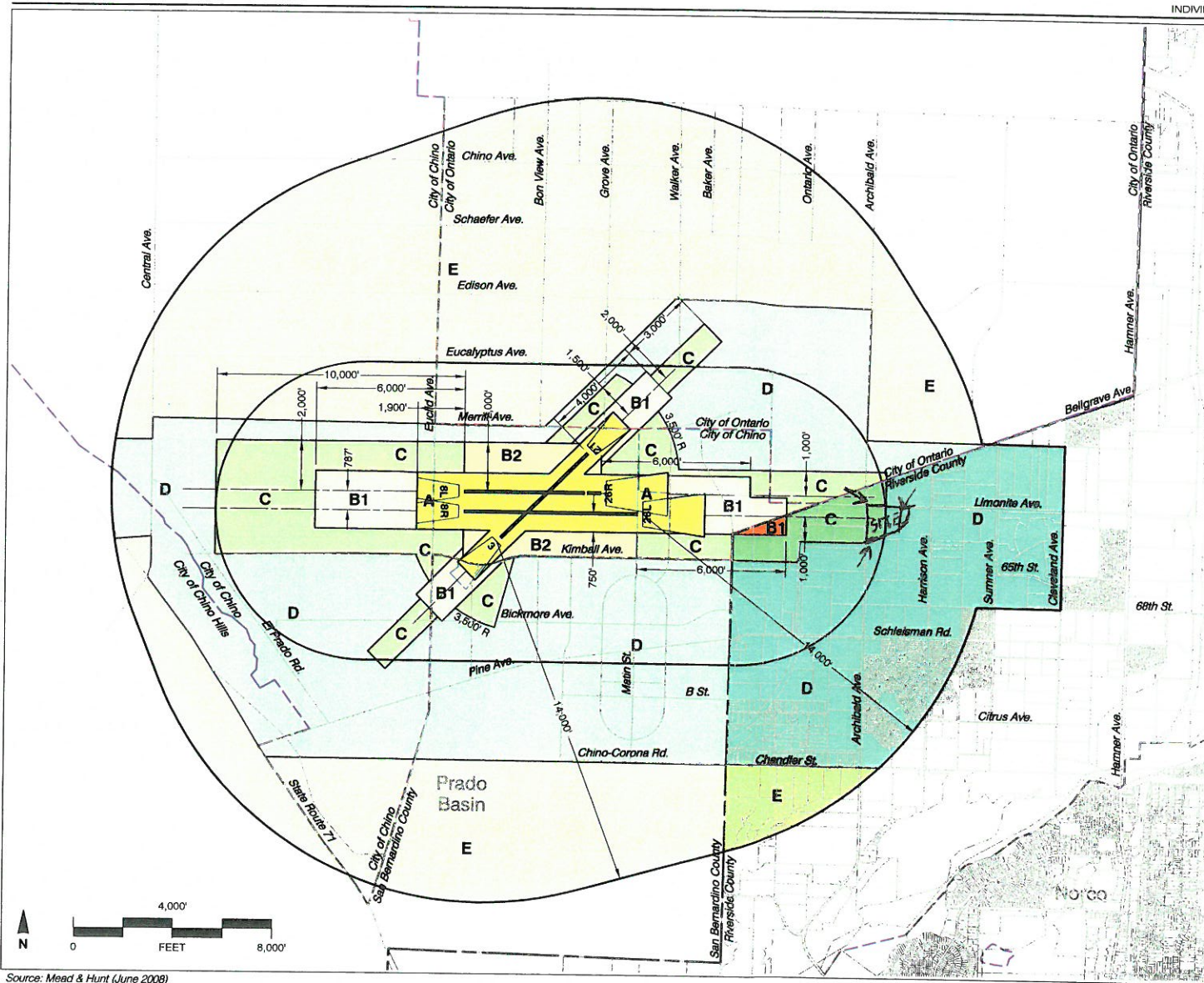
deck areas, and other uses that would be considered to have an occupancy level greater than one person per 30 square feet (minimum square feet per occupant less than 30) pursuant to the California Building Code (1998) Table 10-A (Table C-1 of Appendix C of the Riverside County Airport Land Use Compatibility Plan).

6. The attached notice shall be provided to all potential purchasers of the property and all potential tenants of the buildings, and shall be recorded as a deed notice.
7. The proposed on-site detention basin shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basin that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)





**Legend**

**Compatibility Zones**

- Airport Influence Area Boundary\*
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E

**Boundary Lines**

- Airport Property Line
- City Limits
- County Line

**Note**

Airport influence boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

See Chapter 2, Table 2A, and the Additional Compatibility Policies for Chino Airport for compatibility criteria associated with this map.

\*The policies in this plan apply only to the portions of the airport influence area lying within Riverside County. Compatibility Zones in San Bernardino County are shown only to provide context for the Riverside County area.

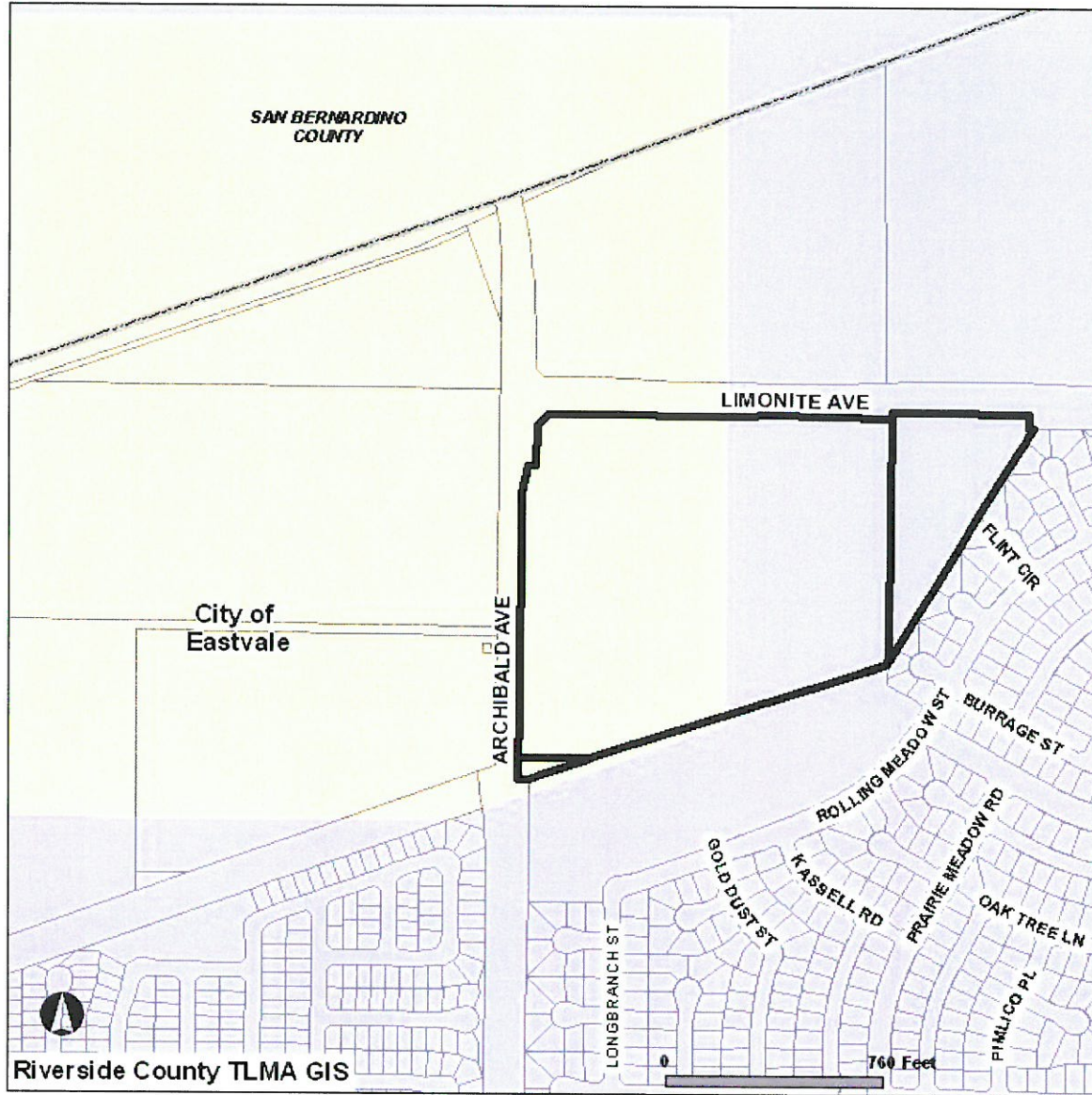
**Riverside County**  
**Airport Land Use Commission**  
**Riverside County**  
**Airport Land Use Compatibility Plan**  
**Policy Document**  
*(Adopted September 2008)*

Map CH-1

**Compatibility Map**  
**Chino Airport**

Source: Mead & Hunt (June 2008)

RIVERSIDE COUNTY GIS



**Selected parcel(s):**

144-030-012 144-030-014 144-030-028

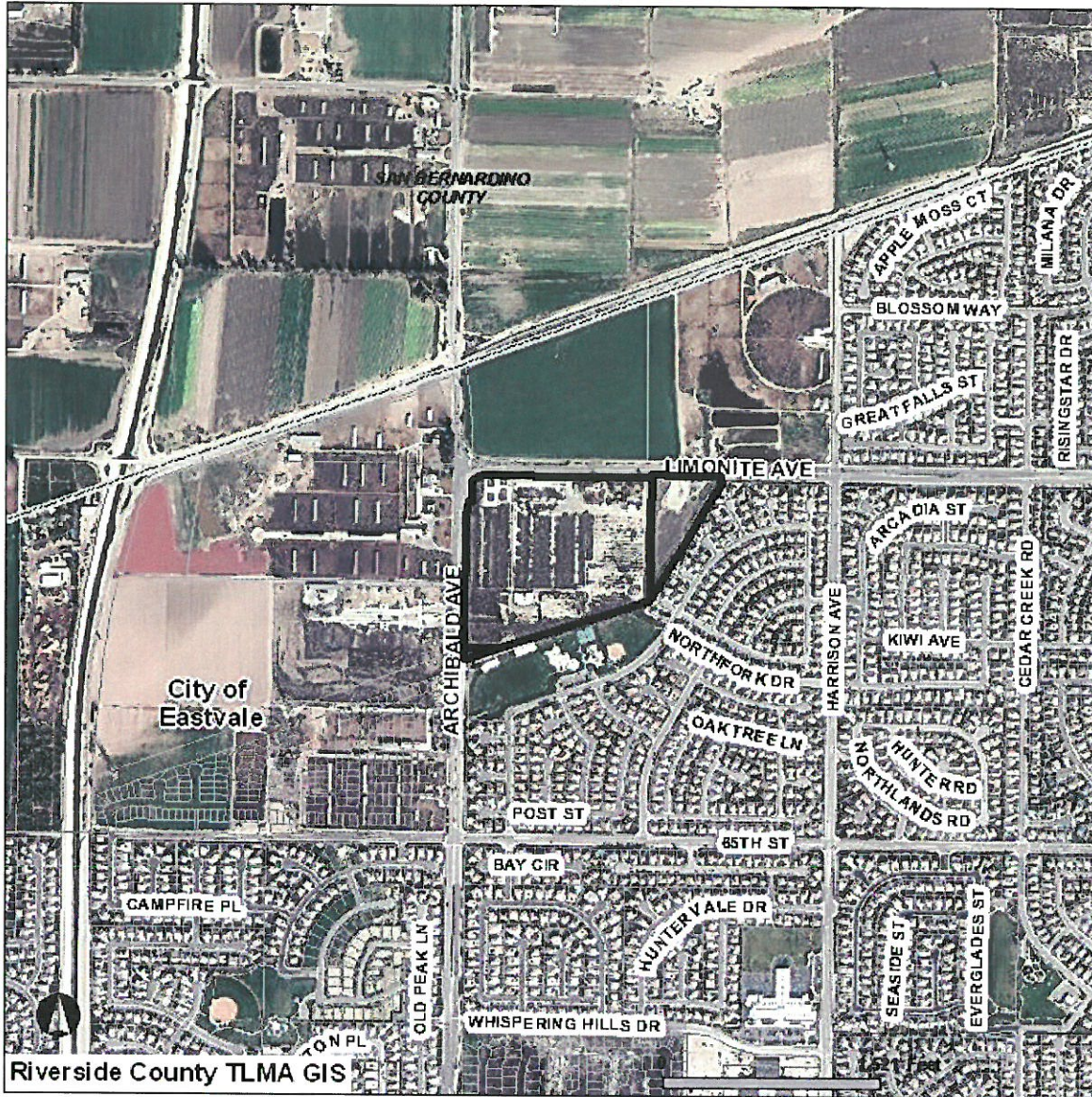
**\*IMPORTANT\***

Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON...Wed Mar 25 09:21:10 2015

Version 131127

RIVERSIDE COUNTY GIS



**Selected parcel(s):**

144-030-012 144-030-014 144-030-028

**\*IMPORTANT\***

Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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RIVERSIDE COUNTY GIS



**Selected parcel(s):**

144-030-012 144-030-014 144-030-028

**\*IMPORTANT\***

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Version 131127

February 25, 2015

VIA HAND DELIVERY

John J. G. Guerin  
Principal Planner  
Riverside County  
Airport Land Use Commission  
County Administrative Center  
4050 Lemon Street, 14<sup>th</sup> Floor  
Riverside, CA 92501

Re: Eastvale Crossings Project Application

Dear John:

On behalf of Wal-Mart Stores, Inc., I would like to submit an Application for Major Land Use Action Review for the proposed Eastvale Crossings Project. The project is set to be located at the southeast corner of Archibald Avenue and Limonite Avenue.

In line with this application, I have attached the following items for your review and consideration:

1. Completed Application with ALTA Survey and Site Plan;
2. Proposed Building Elevations;
3. Floor Plan with square footage breakdown of each area;
4. Site Plan with proposed Walmart in relation to Airport Zoning designations;
5. Open Space Exhibit;
6. Letter and Matrix from Counts Unlimited for Survey conducted at Walmart Ontario;
7. Building Design Features from project architect, Perkowitz & Ruth Architects;

*Since 1910*

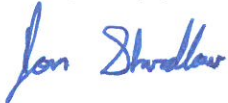
RIVERSIDE 3750 University Avenue, Suite 250 · Riverside, California 92501  
SAN BERNARDINO 550 East Hospitality Lane, Suite 300 · San Bernardino, California 92408  
SAN DIEGO 550 West C Street, Suite 1810 · San Diego, California 92101  
LOS ANGELES 333 South Hope Street, 35th Floor · Los Angeles, California 90071

John J. G. Guerin, Principal Planner  
Riverside County Airport Land Use Commission  
February 25, 2015  
Page 2

8. Walmart Verification of Employees at proposed Store;
9. Landscaping Narrative;
10. Noticing Package;
11. Check in the amount of \$1,188.00.

Please let me know if you need anything else. I can be reached by email at [Jonathan.Shardlow@greshamsavage.com](mailto:Jonathan.Shardlow@greshamsavage.com) or by calling me at (909) 723-1770.

Very truly yours,



Jonathan E. Shardlow, for  
GRESHAM SAVAGE  
NOLAN & TILDEN,  
A Professional Corporation

JES:crb

Enclosure

cc: Ellen Berkowitz, Esq.

APPLICATION FOR  
**Major Development Plan**  
**Conditional Use Permit**  
**Change of Zone**  
**General Plan Amendment**  
**Tentative Tract Map**  
**Sign Program**

FOR

Walmart Store #3129-00

**Applicant:** Wal-Mart Real Estate Business Trust  
**Project Title:** Eastvale Crossings  
**Project Location:** SEC of Archibald Avenue and Limonite Avenue,  
City of Eastvale, California 92880

**Date:** October 22, 2014

**PROJECT PROPOSAL**

**Project Summary and Description**

The overall proposed development includes six parcels consisting of 24.8 gross acres and is bounded by Archibald Avenue to the west; Limonite Avenue, to the north; and a Southern California Edison easement over 2.18 acres to the east and south in the City of Eastvale (the "City"). The James C. Huber Park abuts the Edison easement area to the south, with an existing single-family residential area beyond. The proposed development site will have approximately 23.26 net acres after street and driveway dedications.

Wal-Mart Real Estate Business Trust, (the "Applicant"), proposes to develop a retail shopping center, including an approximately 192,000 square foot Walmart store and four outparcels consisting of: (1) a gas station with an approximately 4,200 square foot convenience store, sixteen (16) fueling positions, and a self-servicing drive-thru carwash at the northeast corner of the site along Limonite Avenue; (2) an approximately 3,500 square foot fast food restaurant with

a drive-thru along Archibald Avenue; (3) a 6,200 square foot retail shop building with a drive-thru and walk-up ATM machine and (4) a 12,200 square foot retail shop building (together, the "Project"). Approximately 1,063 at grade parking stalls would be provided. The proposed Project height would be a maximum of 32-feet (32') with architectural features.

### **Requested Actions**

The Applicant requests approval of the following in order to allow for the development of the Project:

- *Certification of the Eastvale Crossings Environmental Impact Report;*
- *General Plan Amendment* to change the split designation of Commercial Retail and Light Industrial to Commercial Retail for the entire site;
- *Change of Zone* to change the split zoning from General Commercial (C-1/C-P) and Manufacturing-Service Commercial (M-SC) to General Commercial (C-1/C-P) for the entire site;
- *Major Development Plan* for the construction of a non-residential building over 5,000 square feet;
- *Conditional Use Permits* to allow the following uses: off-site alcohol sales (beer, wine and liquor for the Walmart), and drive-thru pharmacy and site-to-store pick-up for the Walmart, gasoline sales with off-site alcohol sales (beer and wine) for the gas station, and drive-thru uses for the retail and fast food restaurant outparcels.
- *Tentative Tract Map* to modify the current lot configuration to the proposed Walmart parcel and subdivide the property into six (6) separate parcels; and
- *Sign Program.*

### **Existing Conditions**

The Project site consists of existing vacant land formerly used as a dairy. Southern California Edison power lines abut the Project site to the east and south. The Project site has natural vegetation consisting of shrubs and the site is relatively flat.

Surrounding uses include vacant land to the north (heavy agriculture zone), a Southern California Edison easement area to the east and south, which separates the Project site from existing single family residences and the James C. Huber park to the south, and vacant land to the west (approved for Industrial Park zone April 9, 2014).

### **Proposed Development**

### **Project Background and Features**



The proposed Project will replace vacant land with a shopping center that provides goods and services for public convenience. The Applicant is proposing an approximately 192,000 square foot Walmart store, along with approximately 26,100 square feet of other smaller retail and restaurant/fast food uses. The Project would be designed in harmony with the existing neighborhood and will minimize impacts on neighboring properties.

The proposed Walmart store will operate 24 hours a day and will sell general retail merchandise, products for the home (i.e. house wares, electronics, furniture and outdoor living items), personal items (i.e. clothing, daily essentials) and groceries (including alcoholic beverages for off-site consumption) displayed in a clean, organized and welcoming environment. The store will also sell pool chemicals, pesticides, paint products and ammunition. The Walmart store may have outdoor seasonal sales and storage. In addition, the store will include a garden center.

The Walmart store will include a pharmacy and may also include a vision, hearing, and medical care center, food service, a photo studio and photo finishing center, a banking center and an arcade and other similar accessory uses inside the store. The store building will include six (6) truck doors and loading docks.

### **Building Design**

Walmart recognizes that the quality and appropriateness of its architecture is its public face and seeks to adapt the design of its proposed Eastvale store comfortably within the context of the surrounding community aesthetics. The structure will be single-story, varying in height from approximately 24'-0" to 32'-0".

The gently arched identity wall, along with the two entrances on the front elevation, announces the customer's destination and provides direction to the appropriate part of the store. The massing of the building's elevations is reduced in scale by the use of pedestrian level glazing, canopies and awnings.

### **Onsite Improvements**

Facing Limonite Avenue, the Project proposes a gas station and facing Archibald Avenue, the Project proposes small retail and restaurant/fast food. The Project will underground the existing power poles and small signs running in the north-south direction along Archibald Avenue. In addition, the Project will provide access to the Southern California Edison easement area running along the eastern and southern boundaries of the Project for maintenance.

### **Site Access**

Retail uses would be located fronting Archibald Avenue and the Walmart and gas station will front Limonite Avenue. The following main customer vehicle access points to the Project are proposed:

- One (1) signalized intersection off of Archibald Avenue;
- One (1) signalized intersection off of Limonite Avenue, and a right-in/right-out driveway off of Limonite Avenue at the eastern end of the Project.

## **Parking**

The City's zoning code requires one (1) space per 200 square feet for retail, four (4) spaces per service bay for automobile service stations, one (1) space per 45 square feet of serving area for restaurants and fast food. A total of 1,052 spaces are required; the Project is proposing 1,063 parking stalls, exceeding the City's parking requirement. Sixteen cart corrals would be provided for the Walmart store. The cart corrals are not included in the total parking count.

## **Landscape**

The Project's landscape design complies with the City's landscaping standards and accommodates the surrounding environment. Plants and materials would be drought tolerant and native California species including trees, shrubs and groundcovers. Landscaping will be provided along the perimeter of the Project site and throughout the site.

## **Detention Basin**

The detention basin is four (4) feet deep with one (1) foot of freeboard, for a total of five (5) feet. The sides of the basin slope down at a maximum 3:1 slope. The volume of the basin is 45,700 CF and the surface area is 14,150 SF at the top of bank. The basin will hold stormwater runoff from the site (Walmart Parcel and four outparcels) prior to discharging into the existing Area Drainage Pipe located in Archibald, adjacent to the site. The basin will also be utilized as a water quality basin and will treat the water quality volume via infiltration.

## **Deliveries**

Delivery trucks for the Walmart store would enter and exit via the signalized intersection along Archibald Avenue as well as the right-in/right-out intersection along Limonite Avenue. The loading areas face Archibald Avenue to the west and the Southern California Edison easement area to the east and provide six loading bays and a compactor. Each loading area would be equipped with roll-down metal screen/security doors.

## **Signage**

Project signage will consist of a combination of letter, logo signs and small projecting pedestrian oriented signs along the retail buildings that are architecturally integrated into the Project design. One center identification monument sign, two business pylon signs, and four business identification monument signs are proposed for the Project.

## **Energy-Efficient Design Features**

### **Energy Conservation & Sustainability**

#### **Lighting**

Sales floor lighting within the Walmart building and exterior lighting within the parking lot will consist of LED lights, which annually consume 34% less energy than a conventional store. Further, the store will include occupancy sensors in most non-sales areas, including restrooms, break rooms, and offices. The sensors automatically turn the light off when the space is unoccupied.

All exterior building signage and many refrigerated food cases will be illuminated with light emitting diodes (LEDs). In refrigerated food cases, LEDs perform well in the cold and produce less heat than fluorescent bulbs – heat which must be compensated for by the refrigeration equipment. LEDs also contain no mercury or lead. LED technology is up to 52 percent more energy efficient than fluorescent lights. Total estimated energy savings for LED lighting in the store’s grocery section is approximately 59,000 kWh per year; enough energy to power five single family homes.

The store will include a daylight harvesting system, which incorporates more efficient lighting, electronic continuous dimming ballasts, skylights and computer controlled daylight sensors that monitor the amount of natural light available. During periods of higher natural daylight, the system dims or turns off the store lights if they are not needed, thereby reducing energy usage. This program will help the store save a substantial amount of energy. Dimming and turning off building lights also helps eliminate unnecessary heat in the building.

#### Central Energy Management System

Walmart employs a centralized energy management system (EMS) to monitor and control the heating, air conditioning, refrigeration and lighting systems for all stores from Walmart’s corporate headquarters in Bentonville, Arkansas. The EMS enables Walmart to constantly monitor and control the store’s energy usage, analyze refrigeration temperatures, observe HVAC and lighting performance, and adjust system levels from a central location 24 hours per day, seven days per week. Energy usage for the store will be monitored and controlled in this manner.

#### Climate Control

The Walmart store will employ one of the industry’s most efficient heating, ventilating and air-conditioning (HVAC) units available. The building will also include a dehumidifying system that allows Walmart to operate the store at a higher temperature, use less energy, and allow the refrigeration system to operate more efficiently.

#### “Cool” Roofs

The Walmart store will utilize a white membrane roof instead of the typical darker colored roof materials employed in commercial construction. The white membrane roof’s higher reflectivity helps reduce building energy consumption and reduces the heat island effect, as compared to buildings utilizing darker roofing colors.

#### Refrigeration

Walmart uses non ozone-depleting refrigerants. It uses R407a for the refrigeration equipment. For air conditioning, Walmart has converted to R410a refrigerant.

Refrigeration equipment is typically roof-mounted close to the refrigerated cases. This reduces the amount of copper refrigerant piping, insulation, potential for leaks and refrigerant charge needed.

#### Heat Reclamation

The Walmart store will reclaim waste heat from onsite refrigeration equipment to supply approximately 70% of the hot water needs for the store.

#### Water Conservation

Walmart will install high-efficiency urinals that use only 1/8 gallon (one pint) of water per flush. This fixture reduces water use by 87 percent compared to the conventional one gallon per flush urinal. The 1/8 gallon urinal also requires less maintenance than waterless urinals, making this the better option for Walmart.

All restroom sinks will use sensor-activated 1/2 gallon per minute high-efficiency faucets. These faucets reduce water usage by approximately 75 percent compared to mandated 1992 EPA Standards. During use, water flows through turbines built into the faucets to generate the electricity needed to operate the motion sensors.

All restroom toilets will be highly efficient and reduce water use. The fixture uses 20 percent less water compared to mandated EPA Standards, of 1.6 gallon per flush fixtures. The toilets utilize built-in water turbines to generate the power required to activate the flush mechanism. These turbines save energy and material by eliminating electrical conduits required to power automatic flush valve sensors.

It is estimated that Walmart's water conservation measures could save up to 530,000 gallons of water annually at this store.

#### Materials and Finishes

**Cement Mixes:** The store will be built using cement mixes that include 15-20 percent fly ash, a waste product of coal-fired electrical generation, or 25-30 percent slag, a by-product of the steel manufacturing process. By incorporating these waste product materials into its cement mixes, Walmart offsets the greenhouse gases emitted in the cement manufacturing process.

The store will use Non-Reinforced Thermoplastic Panel (NRP) in lieu of Fiber Reinforced Plastic (FRP) sheets on the walls in areas where plastic sheeting is appropriate, including food preparation areas, utility and janitorial areas, and associate break rooms. NRP can be recycled, has better impact resistance and, like FRP, is easy to keep clean.

The store will use a plant based oil extracted from a renewable resource as a concrete form release agent (a product sprayed on concrete forms to allow ease of removal after the concrete has set). This release agent is non-petroleum based non-toxic and a biodegradable agent.

For the store's exterior and interior field paint coatings, Walmart will use low volatile organic compound (VOC) paint.

Paint products required for the Project will be primarily purchased in 55 gallon drums and 275 gallon totes, reducing the number of one gallon and five gallon buckets needed. These plastic buckets are filled from the drums and totes and then returned to the paint supplier for cleaning and reuse.

Exposed concrete stores are used "to reduce surface applied flooring materials", eliminating the need for most chemical cleaners, wax strippers and propane-powered buffing.

#### Recycled Building Materials

Construction of the store will use steel containing approximately 90-98 percent recycled structural steel, which utilizes less energy in the mining and manufacturing process than does new steel.

All of the plastic baseboards and much of the plastic shelving included in the expansion area will be composed of recycled plastic.

#### Construction and Demolition (C&D) Recycling

Walmart will employ a Construction and Demolition (C&D) program at this location in order to capture and recycle as much of the metals, woods, floor and ceiling tiles, concretes, asphalts and other materials generated as part of Walmart's demolition and construction process as possible. Walmart will work with a waste management company to fully research all available C&D recycling facilities in the area, and its C&D program will seek to include the widest possible range of materials recovery options.

#### **Building Materials**

The project features material and finish in a variety of textures and warm rich earth tone palette.

#### **Mechanical Equipment**

To further enhance the building design, the Project attractively and effectively conceals outdoor storage, cart storage, and truck loading by using screen walls articulated to match the main building. Rooftop equipment is screened from view by parapet walls of all Project buildings.

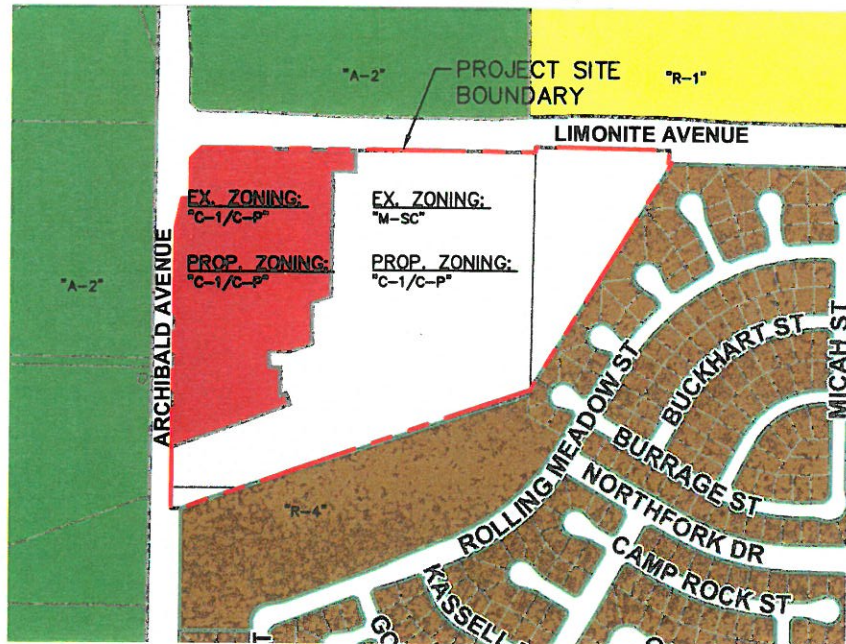
# WALMART STORE 3129-00

EASTVALE, CA

## LEGEND:

Low Density Residential	Business Park	Eastvale City Boundary
Medium Density Residential	Public Facilities	Other City Boundary
Medium High Density Residential	Agriculture	Riverside County Boundary
High Density Residential	Conservation	Airport Influence Areas*
Highest Density Residential	Open Space Recreation	Proposed Amendment of Land Use Designation to Commercial Retail
Commercial Retail	Water	
Light Industrial	Freeway	

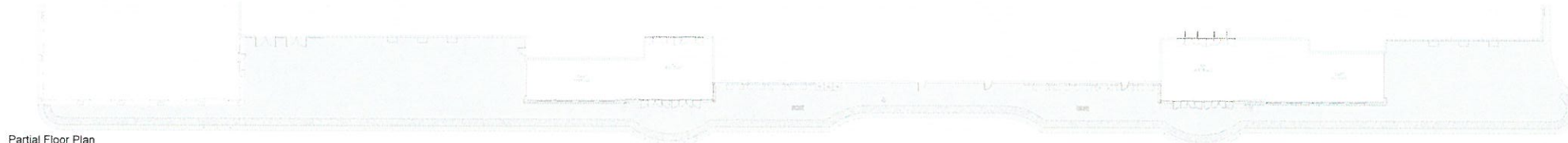
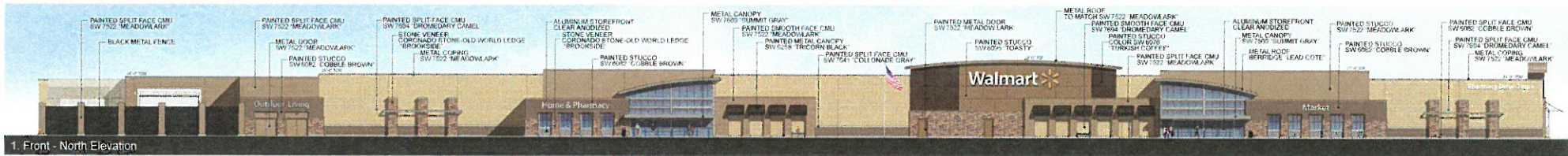
## SITE SPECIFIC ZONING MAP:



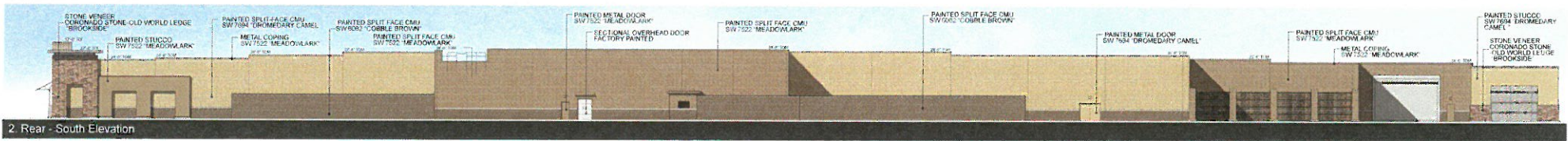
VICINITY MAP

ZONING AMENDMENT EXHIBIT

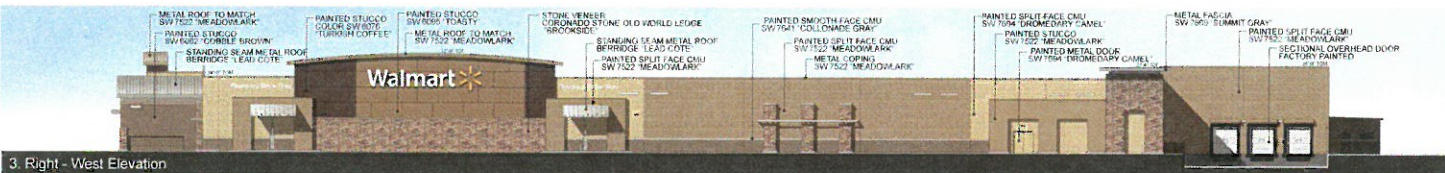




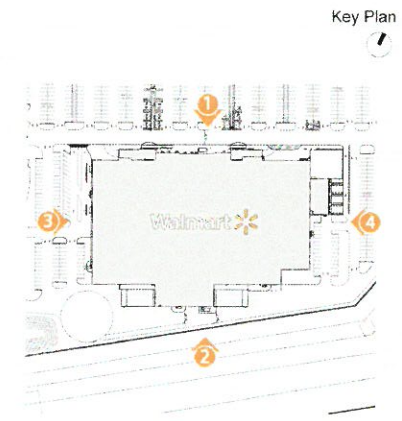
Partial Floor Plan



2. Rear - South Elevation



3. Right - West Elevation

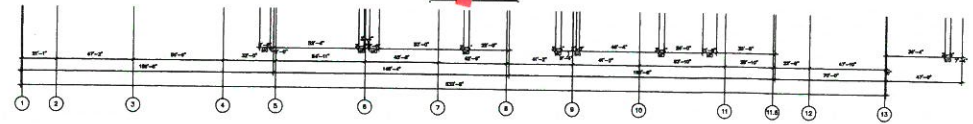
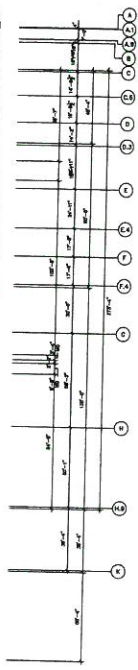
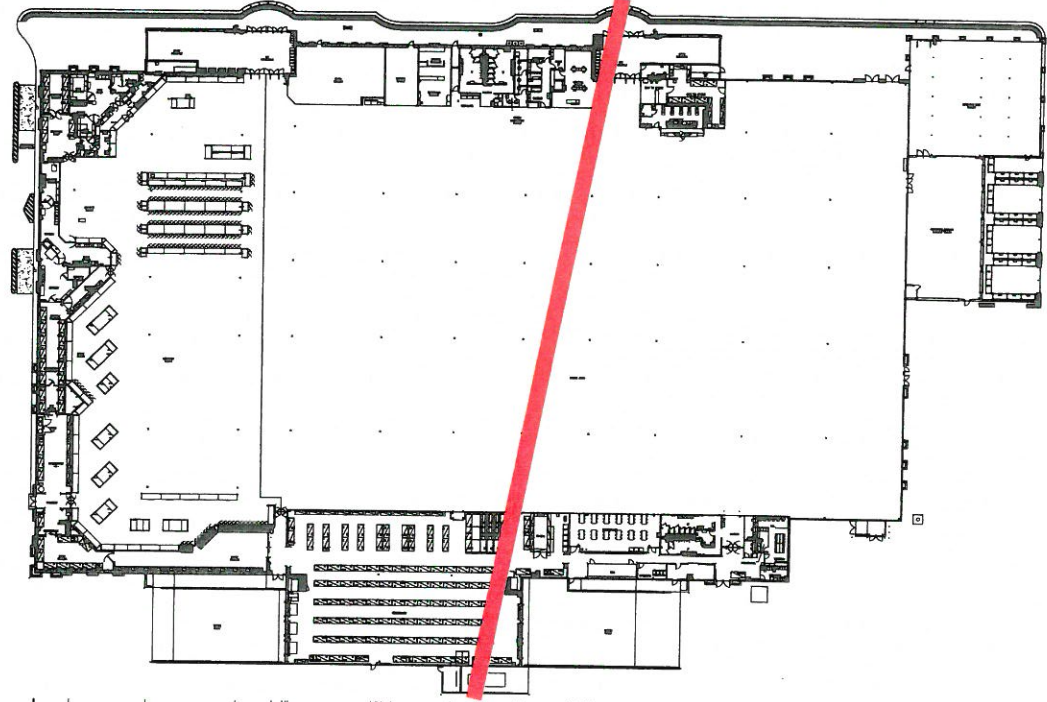
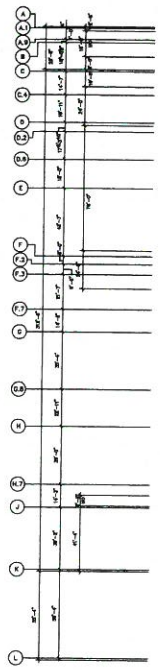
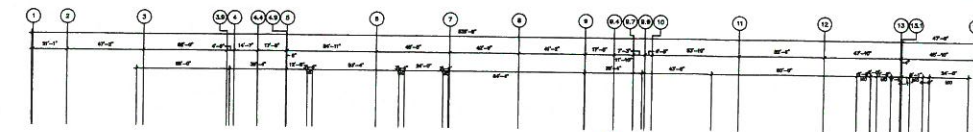


Key Plan



4. Left - East Elevation





**BUILDING SQUARE FOOTAGE BREAKDOWN**

GENERAL MERCHANDISE SALES AREA	103,224 SQ FT
GROCERY SALES AREA	32,451 SQ FT
GROCERY SUPPORT AREA	11,777 SQ FT
TLE SERVICE & SUPPORT AREA	N/A
TLE RETAIL SALES AREA	N/A
RETAIL TENANT AREA	5,130 SQ FT
STOCKROOM/RECEIVING AREA	17,842 SQ FT
ANCILLARY AREA	7,495 SQ FT
GM FRONT CANOPY AREA	1,863 SQ FT
GROCERY FRONT CANOPY AREA	2,265 SQ FT
SEASONAL/INDOOR GARDEN SALES AREA	4,260 SQ FT
OUTDOOR GARDEN CENTER AREA	5,782 SQ FT
<b>GROSS LEASABLE AREA (EXCLUDING OUTDOOR CENTER, SEASONAL GARDEN CENTER, GR FRONT CANOPY, GM FRONT CANOPY)</b>	<b>177,719 SQ FT</b>
<b>TOTAL BUILDING AREA (INCLUDING SEASONAL GARDEN CENTER, GR FRONT CANOPY, GM FRONT CANOPY, BUT EXCLUDING OUTDOOR GARDEN CENTER)</b>	<b>185,907 SQ FT</b>
<b>TOTAL AREA (INCLUDING OUTDOOR GARDEN CENTER)</b>	<b>191,669 SQ FT</b>

**SITE AMENITIES**

SYMBOL	QTY.	DESCRIPTION	WM	GC
○		DECORATIVE BOLLARD WITH GRAY SLEEVE REF SPECIFICATIONS (IMPACT BOLLARDS NOT INCLUDED)	WM	GC
⊙		TRASH CAN REF SPECIFICATIONS	WM	GC
■		BENCH REF SPECIFICATIONS	WM	GC
■		BIKE RACKS (EACH RACK = 5 BIKES, EXCEPT AT BIKE STORAGE RACK = 7 BIKES) REF SPECIFICATIONS	WM	GC

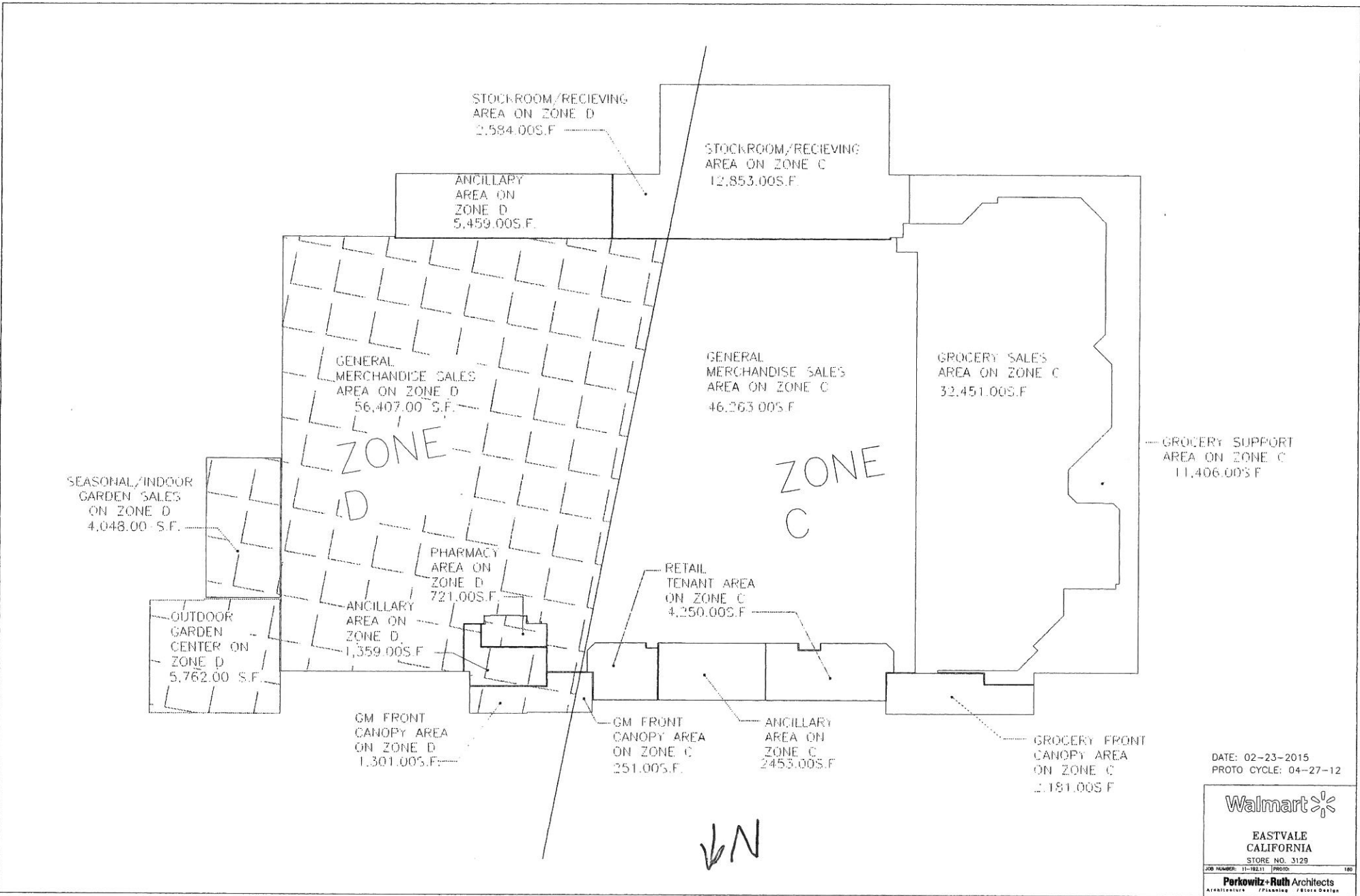
ZONE C DELINEATING LINE

DATE: 06-06-2013  
PROTO CYCLE: 04-27-12

**Walmart**

**EASTVALE  
CALIFORNIA**  
STORE NO. 3129

**Perkins+Roth Architects**  
ARCHITECTS



DATE: 02-23-2015  
 PROTO CYCLE: 04-27-12



EASTVALE  
 CALIFORNIA  
 STORE NO. 3129



February 24, 2015

Riverside County Administrative Center  
Airport Land Use Commission  
Attn: John Guerin  
4080 Lemon Street, 14th Floor  
Riverside, CA 92501

**RE: *Eastvale, CA – Walmart Store #3129-00***

Dear Mr. Guerin:

The open space area exhibit, dated February 23, 2015 was submitted to the Airport Land Use Commission (ALUC) by Kimley Horn and Associates and depicts areas which will meet ALUC's open space area requirements. All uses within these areas will comply with the requirements of Policy 4.2.4.

Pursuant to Policy 4.2.4(a), to qualify as open land, an area should be (1) free of most structures and other major obstacles such as walls, large trees or poles (greater than 4 inches in diameter, measured 4 feet above the ground), and overhead wires, and (2) have minimum dimensions of approximately 75 feet by 300 feet. Bushes and hedges will be used in these areas, as opposed to trees, and maintained at a height of less than 4 feet. Placement of trees on the site, excluding open space areas, will be sited to prevent the occurrence of the formation of a continuous canopy. In addition, all landscaping on-site will utilize species which do not produce seeds, fruits, or berries so as to not unnecessarily attract birds.

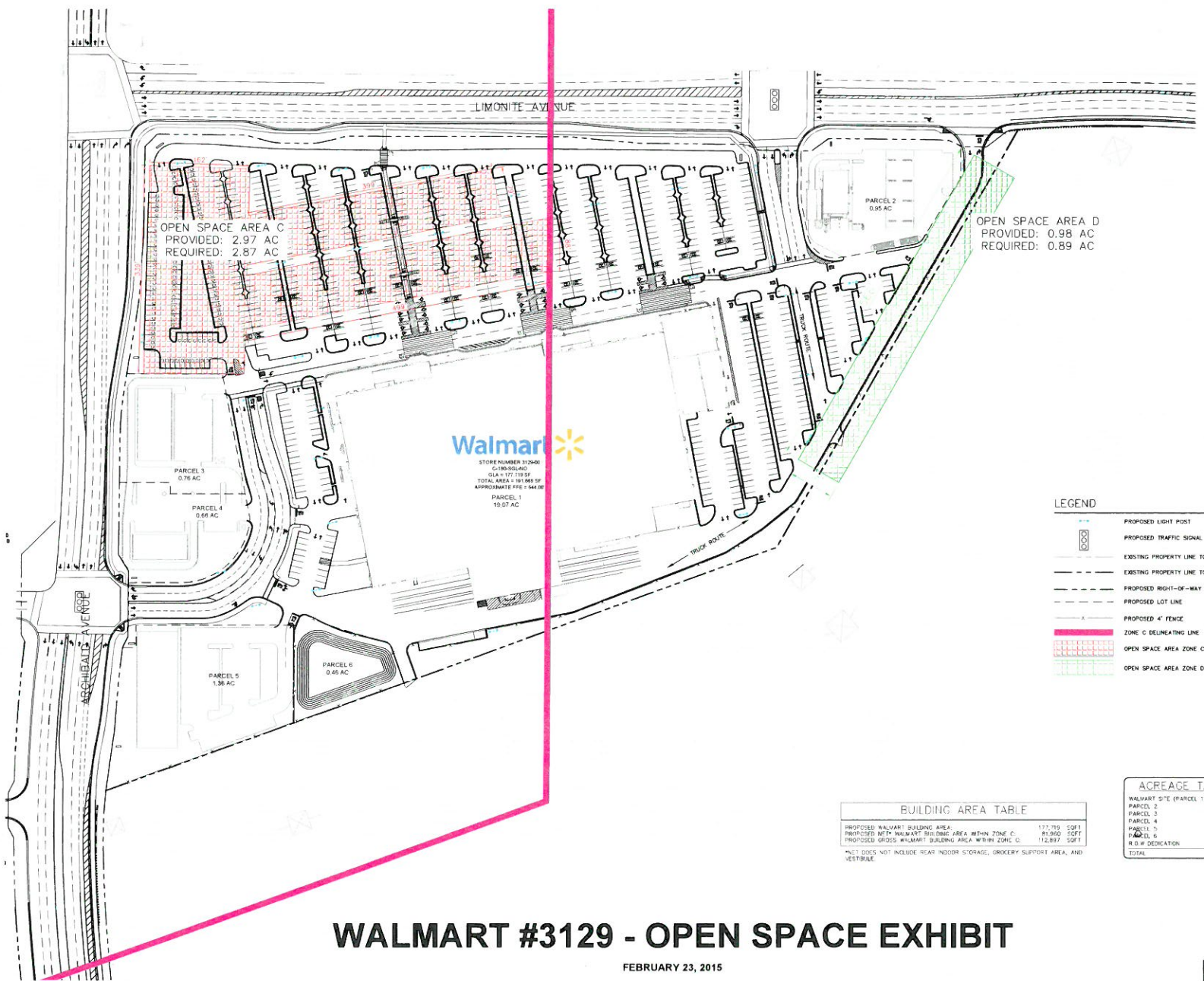
Please contact me at (714) 705-1343 or [kevin.roberson@kimley-horn.com](mailto:kevin.roberson@kimley-horn.com) should you have any questions.

Sincerely,

A handwritten signature in black ink, appearing to read "Kevin M. Roberson".

Kevin M. Roberson, P.E.

Vice President













OPEN SPACE AREA C  
 PROVIDED: 2.97 AC  
 REQUIRED: 2.87 AC

OPEN SPACE AREA D  
 PROVIDED: 0.98 AC  
 REQUIRED: 0.89 AC

**Walmart** 

STORE NUMBER: 31290  
 C-19055-002  
 GLA = 177,719 SF  
 TOTAL AREA = 191,989 SF  
 APPROXIMATE PFE = 644.07  
 PARCEL 1: 19.07 AC

- LEGEND**
-  PROPOSED LIGHT POST
  -  PROPOSED TRAFFIC SIGNAL
  -  EXISTING PROPERTY LINE TO BE ABANDONED
  -  EXISTING PROPERTY LINE TO REMAIN
  -  PROPOSED RIGHT-OF-WAY LINE
  -  PROPOSED LOT LINE
  -  PROPOSED 4' FENCE
  -  ZONE C DELINEATING LINE
  -  OPEN SPACE AREA ZONE C
  -  OPEN SPACE AREA ZONE D

**BUILDING AREA TABLE**

PROPOSED WALMART BUILDING AREA:	177,719 SQ FT
PROPOSED NET WALMART BUILDING AREA WITHIN ZONE C:	81,960 SQ FT
PROPOSED GROSS WALMART BUILDING AREA WITHIN ZONE C:	112,897 SQ FT

\*NET DOES NOT INCLUDE REAR INDOOR STORAGE, GROCERY SUPPORT AREA, AND VESTIBULE.

**ACREAGE TABLE**

WALMART SITE (PARCEL 1)	19.07 AC
PARCEL 2	0.95 AC
PARCEL 3	0.76 AC
PARCEL 4	0.66 AC
PARCEL 5	1.36 AC
PARCEL 6	0.45 AC
R.O.W. DEDICATION	1.52 AC
TOTAL	24.76 AC

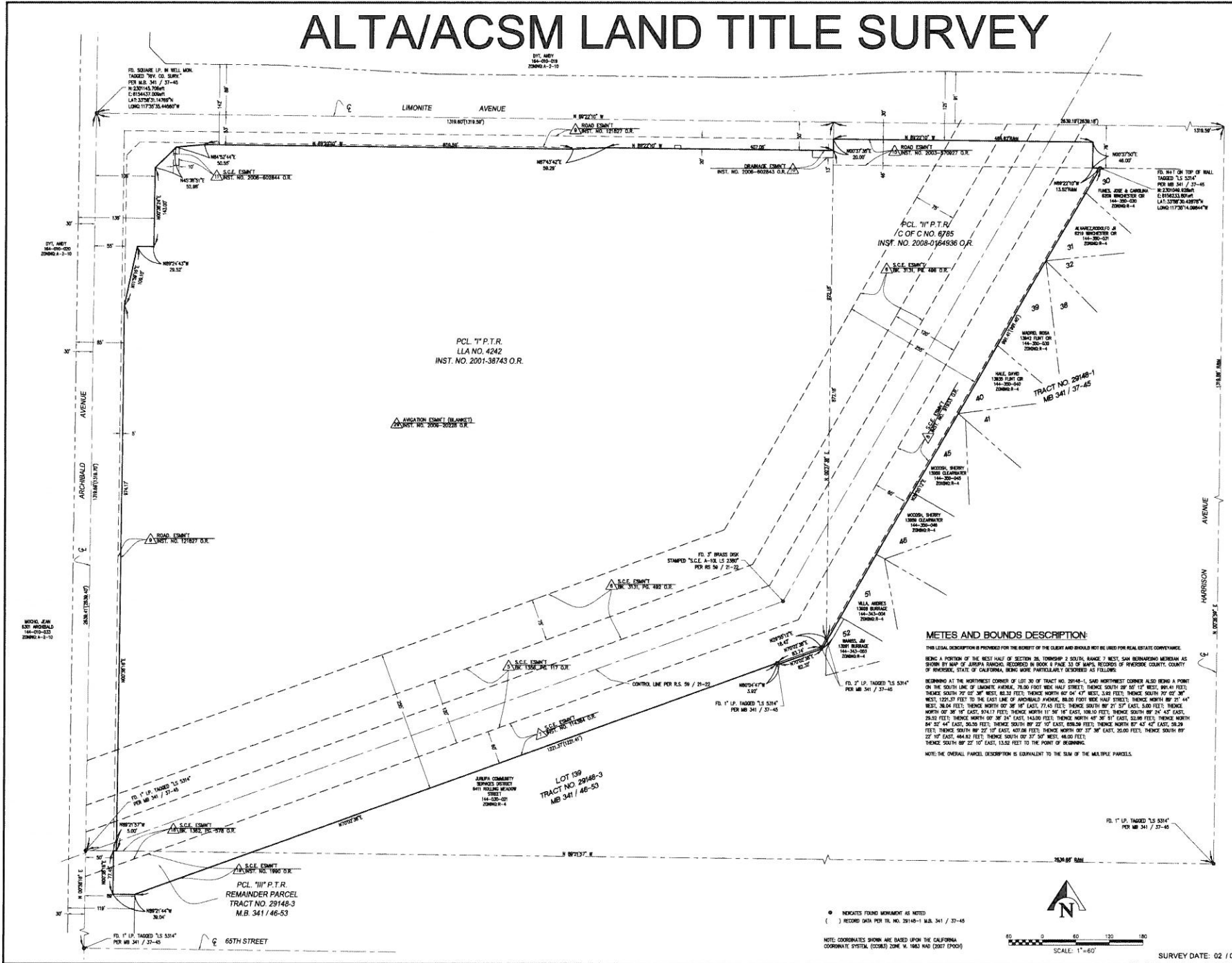


# WALMART #3129 - OPEN SPACE EXHIBIT

FEBRUARY 23, 2015



# ALTA/ACSM LAND TITLE SURVEY



ALTA/ACSM LAND TITLE SURVEY  
WAL-MART STORE #70545-00  
EASTVALE, CALIFORNIA  
PREPARED FOR KIMLEY-HORN

JOB NO.  
**11-004**  
SHEET  
**2**  
OF 6 SHEETS

**Kimley-Horn and Associates, Inc.**  
Engineering, Planning, and Environmental Consultants © 2008  
760 The City Drive, Suite 400  
Irvine, California 92618  
Tel: (949) 251-1333  
Fax: (949) 251-1300

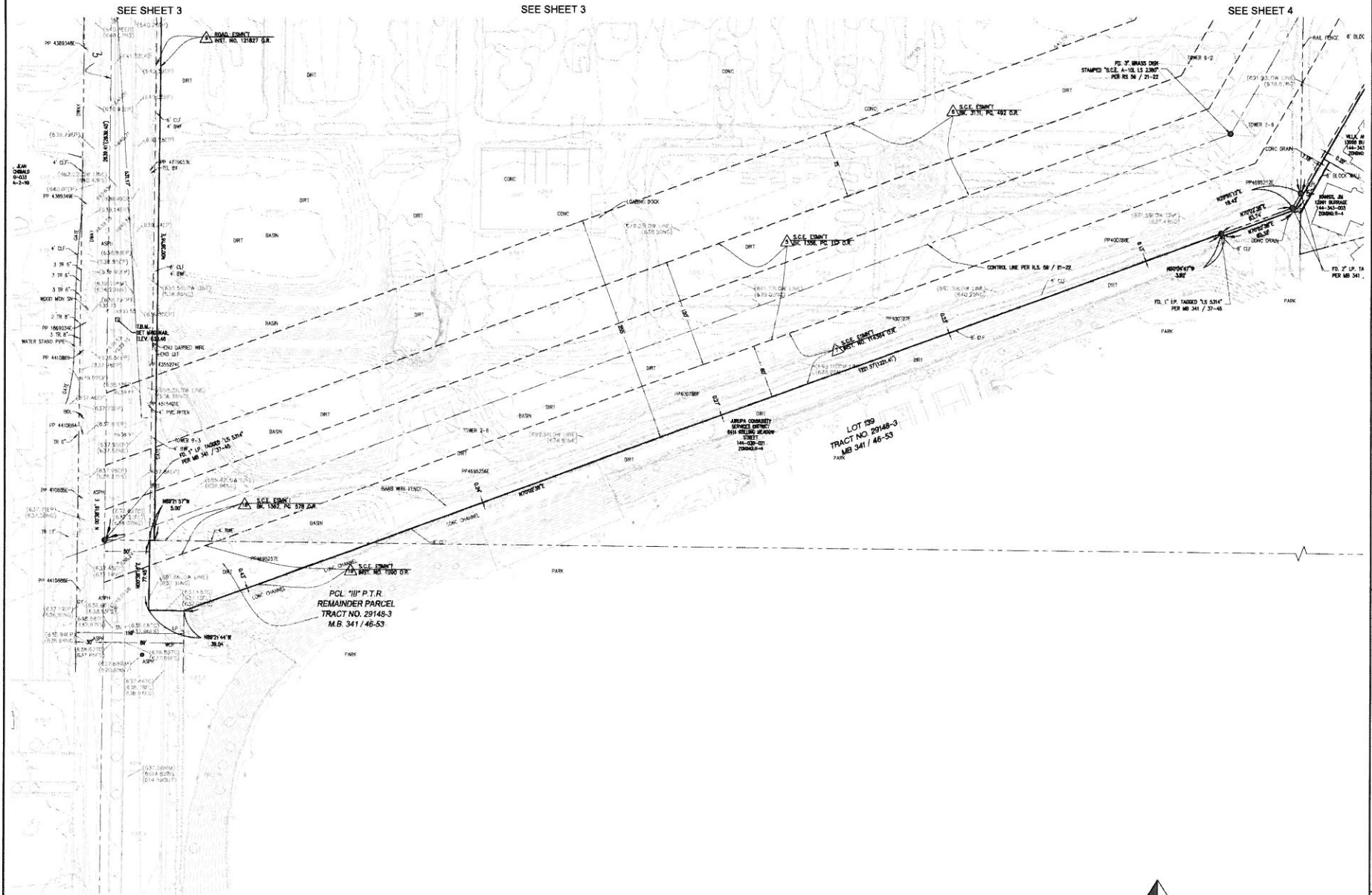
SURVEY DATE: 02 / 07 / 2011







# ALTA/ACSM LAND TITLE SURVEY

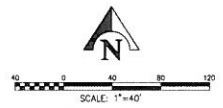


REVISION DESCRIPTION
NO.
DATE

**Kimley-Horn and Associates, Inc.**  
 Engineering, Planning, and Environmental Consultants © 2008  
 10000 Wilshire Blvd., Suite 400  
 Los Angeles, California 90024  
 Tel: (310) 407-4600  
 Fax: (310) 407-4601

**ALTA/ACSM LAND TITLE SURVEY**  
 WAL-MART STORE #70545-00  
 EASTVALE, CALIFORNIA  
 PREPARED FOR: KIMLEY-HORN

JOB NO.	11-004
SHEET	5
OF 8 SHEETS	



SURVEY DATE: 02 / 07 / 2011



### WALMART CUSTOMER COUNTS

Counts Unlimited, Inc. was retained to conduct customer counts at the Ontario Walmart located at 1333 North Mountain Avenue, Ontario, CA 91762 for use as part of a Riverside Airport Land Use Commission Survey of Similar Uses. Customer counts were conducted on the following days and hours:

1pm-3pm Saturday, January 17

4pm-6pm Sunday, January 18

7am-9am Monday, January 19

4pm-6pm Tuesday, January 20

7am-9am Wednesday, January 21

4pm-6pm Thursday, January 22

1pm-3pm Friday, January 23

4pm-6pm Saturday, January 24

1pm-3pm Sunday, January 25

In order to conduct an accurate customer count, approximately 5 minutes before the count start times detailed above, four (4) Counts Unlimited, Inc. associates conducted customer counts of the customers located in the store to arrive at a "starting occupancy." Thereafter, these same associates positioned themselves at the entrances and exits of the building at the count start time. Utilizing Jamar DB-400 Electronic Pedestrian Count Boards, entering and exiting customers were then counted so that a running total could be established for each successive minute in the two hour timeframe. The results of the customer counts were subsequently compiled into a "Pedestrian Occupancy Count" spreadsheet. Please do not hesitate to contact us if you have any questions.

Sincerely,

A handwritten signature in black ink, appearing to read "Kris Campos".

Kris Campos  
Project Manager

Walmart Supercenter 1333 North Mountain Ave Ontario, CA 91762	Market Area Enter	Market Area Exit	Pharmacy Area Enter	Pharmacy Area Exit	Garden Area Enter	Garden Area Exit	Running Total
Starting Occupancy							495
13:00	7	4	11	13	2	0	498
13:01	9	6	6	9	1	0	499
13:02	7	3	7	9	0	0	501
13:03	10	8	12	8	0	0	507
13:04	5	11	8	7	0	2	500
13:05	7	4	4	13	2	0	496
13:06	8	1	8	7	3	0	507
13:07	11	7	4	9	0	1	505
13:08	7	12	9	9	1	2	499
13:09	4	6	7	3	0	3	498
13:10	0	8	11	11	2	1	491
13:11	7	10	8	7	5	0	494
13:12	6	7	2	8	1	1	487
13:13	8	8	9	7	2	0	491
13:14	10	13	1	9	4	0	484
13:15	8	12	7	6	0	0	481
13:16	9	9	11	4	0	1	487
13:17	7	13	9	8	2	1	483
13:18	8	10	5	3	3	1	485
13:19	10	9	8	5	1	1	489
13:20	9	11	4	4	0	0	487
13:21	6	12	6	9	0	0	478
13:22	11	10	7	3	1	0	484
13:23	10	7	10	5	2	2	492
13:24	8	13	7	3	0	0	491
13:25	9	11	11	10	2	0	492
13:26	7	8	8	3	2	1	497
13:27	9	8	5	7	1	0	497
13:28	12	12	5	9	0	2	491
13:29	9	7	10	6	1	0	498
13:30	8	7	8	8	1	0	500
13:31	7	7	6	8	0	0	498
13:32	6	6	8	11	0	0	495
13:33	6	7	4	4	0	1	493
13:34	7	9	11	9	2	0	495
13:35	6	10	5	10	0	0	486
13:36	3	6	8	3	4	2	490
13:37	6	9	6	8	1	2	484
13:38	9	8	2	7	0	0	480
13:39	5	10	9	14	2	0	472
13:40	5	4	5	9	0	1	468
13:41	8	9	6	5	0	0	468
13:42	4	4	4	12	4	0	464
13:43	5	5	5	8	0	2	459
13:44	8	9	3	11	2	0	452
13:45	6	6	9	8	0	0	453
13:46	6	6	9	9	0	0	453
13:47	8	7	11	7	0	2	456
13:48	7	6	10	8	0	0	459
13:49	6	7	6	8	1	0	457
13:50	6	8	7	12	0	3	447
13:51	3	8	13	8	2	0	449
13:52	5	6	6	7	0	1	446
13:53	2	4	4	10	2	0	440
13:54	4	2	10	13	0	0	439
13:55	7	2	11	9	0	2	444
13:56	2	7	6	11	0	0	434
13:57	6	9	13	7	1	0	438
13:58	9	5	9	8	0	1	442
13:59	6	8	5	8	0	0	437
14:00	7	4	7	8	3	0	442
14:01	6	4	11	3	1	0	453
14:02	4	9	14	4	0	0	458
14:03	8	6	9	9	0	1	459
14:04	2	4	10	6	5	2	464
14:05	9	7	8	6	0	0	468
14:06	3	3	8	6	2	0	472
14:07	1	1	11	7	4	4	476
14:08	5	3	10	5	0	0	483
14:09	2	2	9	8	2	3	483
14:10	2	2	3	10	1	0	477
14:11	8	5	6	7	3	0	482
14:12	0	5	8	13	4	2	474
14:13	6	4	3	12	4	0	471
14:14	4	5	4	9	1	0	466
14:15	7	5	6	12	0	0	462
14:16	2	5	10	3	0	0	466
14:17	5	4	5	15	2	2	457
14:18	2	4	11	10	0	0	456
14:19	2	7	8	12	1	2	446
14:20	0	4	4	6	3	0	443
14:21	4	2	7	9	4	0	447

Walmart Supercenter 1333 North Mountain Ave Ontario, CA 91762	Market Area Enter	Market Area Exit	Pharmacy Area Enter	Pharmacy Area Exit	Garden Area Enter	Garden Area Exit	Running Total
14:22	4	9	8	10	0	0	440
14:23	3	6	8	10	0	0	435
14:24	5	0	4	7	0	1	436
14:25	3	8	6	13	2	0	426
14:26	4	2	7	7	0	3	425
14:27	0	4	3	10	2	0	416
14:28	0	3	5	6	0	0	412
14:29	2	8	4	8	0	0	402
14:30	5	15	7	7	1	0	393
14:31	7	7	4	9	1	0	389
14:32	4	4	8	4	0	2	391
14:33	3	10	8	8	2	0	386
14:34	4	3	2	3	2	0	388
14:35	8	9	9	5	0	3	388
14:36	7	4	4	9	2	0	388
14:37	2	8	3	7	4	2	380
14:38	10	8	2	10	0	0	374
14:39	4	4	7	7	5	2	377
14:40	9	7	8	6	1	2	380
14:41	4	3	5	6	0	0	380
14:42	11	5	5	4	2	0	389
14:43	7	7	7	4	0	2	390
14:44	3	1	4	8	0	0	388
14:45	7	2	6	7	2	1	393
14:46	9	5	4	10	2	0	393
14:47	4	4	8	9	1	0	393
14:48	8	11	9	4	0	2	393
14:49	4	6	11	12	0	0	390
14:50	5	4	7	8	0	1	389
14:51	5	5	7	3	0	0	393
14:52	5	2	13	9	1	0	401
14:53	7	1	9	4	2	0	414
14:54	5	4	10	10	0	0	415
14:55	6	6	7	9	0	2	411
14:56	3	8	12	7	0	0	411
14:57	4	9	6	7	0	3	402
14:58	7	12	13	2	2	0	410
14:59	2	7	14	6	0	0	413
Total	693	777	875	929	134	78	

Walmart Supercenter							
1333 North Mountain Ave Ontario, CA 91762	Market Area Enter	Market Area Exit	Pharmacy Area Enter	Pharmacy Area Exit	Garden Area Enter	Garden Area Exit	Running Total
Starting Occupancy							542
16:00	8	10	10	8	0	1	541
16:01	11	10	8	11	0	0	539
16:02	8	7	2	4	1	0	539
16:03	7	9	7	7	2	0	539
16:04	4	7	9	4	0	2	539
16:05	13	12	4	9	0	0	535
16:06	8	8	7	9	1	1	533
16:07	11	9	4	6	0	0	533
16:08	6	4	7	8	0	0	534
16:09	9	10	9	6	2	0	538
16:10	10	7	11	4	3	0	551
16:11	8	3	8	7	0	1	556
16:12	4	9	10	12	1	4	546
16:13	9	4	4	7	0	0	548
16:14	7	8	5	9	2	0	545
16:15	7	8	6	9	1	0	542
16:16	9	9	11	12	0	4	537
16:17	12	10	2	4	0	0	537
16:18	7	7	9	7	2	2	539
16:19	9	3	4	11	1	0	539
16:20	4	6	7	11	2	0	535
16:21	13	5	4	7	0	3	537
16:22	8	9	7	12	0	0	531
16:23	9	1	13	9	3	2	544
16:24	11	10	9	4	2	1	551
16:25	7	4	2	8	0	4	544
16:26	5	13	5	11	2	2	530
16:27	9	9	14	7	0	2	535
16:28	4	12	9	6	0	3	527
16:29	5	4	14	4	2	0	540
16:30	9	10	4	9	2	0	536
16:31	13	5	9	7	1	0	547
16:32	7	8	8	4	0	0	550
16:33	12	10	4	11	0	0	545
16:34	11	7	7	5	4	0	555
16:35	13	12	4	9	0	2	549
16:36	9	4	9	4	2	0	561
16:37	4	9	3	10	0	0	549
16:38	8	10	10	6	0	0	551
16:39	9	5	4	4	0	1	554
16:40	4	6	9	9	3	0	555
16:41	6	4	4	3	2	0	560
16:42	3	13	8	10	2	2	548
16:43	2	4	3	4	3	0	548
16:44	2	8	3	4	0	0	541
16:45	9	11	7	4	2	0	544
16:46	7	8	10	11	0	0	542
16:47	12	16	4	4	0	0	538
16:48	8	9	9	9	4	0	541
16:49	9	10	13	10	0	0	543
16:50	6	6	7	11	0	2	537
16:51	8	2	4	7	2	0	542
16:52	9	7	8	15	6	0	543
16:53	13	11	7	12	3	0	543
16:54	13	9	4	9	0	0	542
16:55	8	8	3	14	0	1	530
16:56	12	6	9	6	0	0	539
16:57	6	11	5	15	3	0	527
16:58	6	7	8	11	0	0	523
16:59	9	14	2	7	0	0	513
17:00	8	13	7	8	1	2	506
17:01	5	8	15	10	0	0	508
17:02	7	10	4	4	0	0	505
17:03	11	4	9	7	2	3	513
17:04	10	9	12	12	0	0	514
17:05	4	13	8	9	0	0	504
17:06	8	11	13	4	3	1	512
17:07	3	6	8	10	0	2	505
17:08	9	8	6	6	0	0	506
17:09	4	12	7	8	3	0	500
17:10	8	4	10	2	0	0	512
17:11	4	9	4	7	2	0	506
17:12	9	10	3	3	0	2	503
17:13	0	9	6	4	1	0	497
17:14	2	8	3	6	1	0	489
17:15	10	9	6	9	0	0	487
17:16	7	13	13	2	0	2	490
17:17	9	10	10	8	0	0	491
17:18	12	5	6	4	0	0	500
17:19	4	13	7	7	2	3	490
17:20	5	7	12	10	0	0	490
17:21	3	12	9	3	0	4	483

Walmart Supercenter 1333 North Mountain Ave Ontario, CA 91762	Market Area Enter	Market Area Exit	Pharmacy Area Enter	Pharmacy Area Exit	Garden Area Enter	Garden Area Exit	Running Total
17:22	4	11	8	9	4	0	479
17:23	10	6	10	2	0	2	489
17:24	2	15	9	8	0	0	477
17:25	16	9	13	4	0	0	493
17:26	11	16	8	7	0	3	486
17:27	8	4	6	6	2	2	490
17:28	10	9	7	12	0	5	481
17:29	6	12	12	9	0	0	478
17:30	13	7	8	8	0	0	484
17:31	8	10	10	10	0	0	482
17:32	4	4	9	4	0	0	487
17:33	6	9	11	2	0	0	493
17:34	7	4	4	9	0	0	491
17:35	7	8	14	9	2	0	497
17:36	10	6	5	4	0	0	502
17:37	9	7	4	11	0	0	497
17:38	4	12	9	15	0	0	483
17:39	8	9	7	8	1	0	482
17:40	12	2	7	2	0	0	497
17:41	8	4	2	9	0	2	492
17:42	3	5	9	4	0	0	495
17:43	8	7	8	7	0	0	497
17:44	6	13	11	11	0	0	490
17:45	9	12	9	14	0	0	482
17:46	8	2	7	7	0	0	488
17:47	2	9	2	12	0	1	470
17:48	7	4	6	9	0	0	470
17:49	4	13	9	13	0	0	457
17:50	9	8	12	15	0	2	453
17:51	3	4	5	12	0	0	445
17:52	5	7	9	2	0	0	450
17:53	12	12	2	7	1	0	446
17:54	7	9	7	12	0	2	437
17:55	2	5	10	9	0	0	435
17:56	4	7	4	13	0	2	421
17:57	9	10	5	11	0	0	414
17:58	6	6	8	2	0	1	419
17:59	6	4	2	9	0	0	414
Total	904	981	876	937	91	81	

Walmart Supercenter 1333 North Mountain Ave Ontario, CA 91762	Market Area Enter	Market Area Exit	Pharmacy Area Enter	Pharmacy Area Exit	Garden Area Enter	Garden Area Exit	Running Total
Starting Occupancy							20
7:00	0	0	0	0	0	0	20
7:01	2	0	0	0	0	0	22
7:02	0	0	0	0	0	0	22
7:03	0	0	2	0	0	0	24
7:04	2	0	0	0	0	0	26
7:05	0	0	0	0	0	0	26
7:06	5	2	1	1	0	0	29
7:07	0	0	0	0	0	0	29
7:08	0	0	0	0	0	0	29
7:09	0	3	1	0	0	0	27
7:10	2	0	0	1	0	0	28
7:11	0	0	0	0	0	0	28
7:12	0	6	1	1	0	0	22
7:13	0	0	0	0	0	0	22
7:14	4	3	0	0	0	0	23
7:15	1	0	0	0	0	0	24
7:16	2	2	0	0	0	0	24
7:17	0	0	0	0	0	0	24
7:18	0	1	0	0	0	0	23
7:19	0	4	0	0	0	0	19
7:20	2	0	2	0	0	0	23
7:21	3	2	0	0	0	0	24
7:22	2	0	0	0	0	0	26
7:23	0	0	2	0	0	0	28
7:24	0	0	0	0	0	0	28
7:25	2	0	0	0	0	0	30
7:26	0	3	0	0	0	0	27
7:27	2	0	0	0	0	0	29
7:28	0	0	0	0	0	0	29
7:29	0	0	0	0	0	0	29
7:30	3	2	2	0	0	0	32
7:31	0	0	1	0	0	0	33
7:32	0	3	0	0	0	0	30
7:33	3	4	2	0	0	0	31
7:34	4	0	3	0	0	0	38
7:35	2	0	0	0	0	0	40
7:36	3	0	3	0	0	0	46
7:37	0	5	0	1	0	0	40
7:38	0	0	0	0	0	0	40
7:39	3	2	2	0	0	0	43
7:40	0	0	0	0	0	0	43
7:41	0	0	2	0	0	0	45
7:42	0	0	0	2	0	0	43
7:43	0	1	2	0	0	0	44
7:44	0	0	4	0	0	0	48
7:45	2	2	1	0	0	0	49
7:46	0	0	0	0	0	0	49
7:47	0	2	0	0	0	0	47
7:48	4	3	1	0	0	0	49
7:49	0	0	0	1	0	0	48
7:50	0	4	0	0	0	0	44
7:51	2	0	2	0	0	0	48
7:52	3	0	0	1	0	0	50
7:53	0	2	3	2	0	0	49
7:54	4	5	0	0	0	0	48
7:55	0	0	1	2	0	0	47
7:56	2	0	3	3	0	0	49
7:57	0	0	3	3	0	0	49
7:58	2	0	0	0	0	0	51
7:59	5	0	0	0	0	0	56
8:00	2	2	0	0	0	0	56
8:01	2	0	0	0	0	0	58
8:02	0	2	0	0	0	0	56
8:03	0	4	2	0	0	0	54
8:04	0	2	0	0	0	0	52
8:05	3	1	0	1	0	0	53
8:06	0	0	0	0	0	0	53
8:07	0	0	2	0	0	0	55
8:08	4	0	1	3	0	0	57
8:09	2	3	0	0	0	0	56
8:10	0	5	3	2	0	0	52
8:11	3	2	0	0	0	0	53
8:12	0	1	0	1	0	0	51
8:13	3	0	0	1	0	0	53
8:14	0	0	0	0	0	0	53
8:15	2	0	2	0	0	0	57
8:16	0	2	1	0	0	0	56
8:17	0	1	0	0	0	0	55
8:18	1	0	3	2	0	0	57
8:19	3	0	0	0	0	0	60
8:20	0	2	2	0	0	0	60
8:21	3	4	4	2	0	0	61

Walmart Supercenter 1333 North Mountain Ave Ontario, CA 91762	Market Area Enter	Market Area Exit	Pharmacy Area Enter	Pharmacy Area Exit	Garden Area Enter	Garden Area Exit	Running Total
8:22	1	0	2	0	0	0	64
8:23	0	2	1	1	0	0	62
8:24	1	1	0	1	2	0	63
8:25	1	2	0	0	0	0	62
8:26	2	2	2	0	0	0	64
8:27	0	0	0	0	0	0	64
8:28	0	3	4	1	0	0	64
8:29	2	2	1	0	0	0	65
8:30	3	3	7	0	0	0	72
8:31	2	1	2	1	0	0	74
8:32	5	0	0	0	0	0	79
8:33	2	1	0	3	0	0	77
8:34	4	0	1	1	0	0	81
8:35	2	0	1	0	0	0	84
8:36	2	0	2	0	0	0	88
8:37	4	2	0	2	0	0	88
8:38	0	3	0	5	1	0	81
8:39	0	0	3	0	0	0	84
8:40	0	1	0	0	0	0	83
8:41	3	4	5	0	0	0	87
8:42	0	0	2	1	0	0	88
8:43	5	2	3	0	0	0	94
8:44	0	4	1	0	0	0	91
8:45	4	2	4	2	0	0	95
8:46	2	0	2	0	0	0	99
8:47	0	4	0	0	0	0	95
8:48	1	0	3	4	0	0	95
8:49	3	1	2	0	0	0	99
8:50	5	0	4	2	0	0	106
8:51	0	3	1	1	0	0	103
8:52	2	4	0	7	2	0	96
8:53	3	1	1	2	0	0	97
8:54	1	0	0	0	0	0	98
8:55	2	1	4	5	0	0	98
8:56	0	2	0	0	0	0	96
8:57	0	0	2	0	0	0	98
8:58	1	4	3	1	0	0	97
8:59	0	2	8	0	0	0	103
Total	162	149	135	70	5	0	



Walmart Supercenter 1333 North Mountain Ave Ontario, CA 91762								Running Total
Market Area Enter	Market Area Exit	Pharmacy Area Enter	Pharmacy Area Exit	Garden Area Enter	Garden Area Exit			
Starting Occupancy							308	
16:00	0	0	8	6	0	0	310	
16:01	0	0	6	8	0	0	308	
16:02	0	0	4	2	1	2	309	
16:03	0	4	2	3	1	0	305	
16:04	4	2	2	4	1	0	306	
16:05	8	8	7	10	0	0	303	
16:06	9	8	13	4	3	0	316	
16:07	6	4	3	8	0	0	313	
16:08	2	8	5	9	2	0	305	
16:09	3	0	5	0	0	0	313	
16:10	5	4	4	4	0	0	314	
16:11	3	5	7	2	3	1	319	
16:12	11	5	5	2	1	2	327	
16:13	4	3	5	8	3	0	328	
16:14	7	4	11	2	0	2	338	
16:15	14	6	1	2	0	0	345	
16:16	8	21	0	11	0	0	321	
16:17	8	13	7	12	0	0	311	
16:18	9	14	3	1	0	0	308	
16:19	1	3	2	8	2	0	302	
16:20	1	3	6	4	0	1	301	
16:21	7	1	4	6	0	0	305	
16:22	8	2	6	7	0	0	310	
16:23	5	10	5	6	1	0	305	
16:24	9	7	3	1	0	0	309	
16:25	7	9	2	4	0	0	305	
16:26	14	3	10	8	2	0	320	
16:27	2	13	2	8	1	2	302	
16:28	4	10	7	3	0	0	300	
16:29	6	8	5	2	0	0	301	
16:30	15	6	9	11	3	0	311	
16:31	3	6	2	2	0	0	308	
16:32	5	7	6	9	0	0	303	
16:33	3	8	3	1	2	0	302	
16:34	8	2	1	3	0	3	303	
16:35	5	2	3	6	0	6	297	
16:36	7	8	5	5	1	3	294	
16:37	4	3	5	9	0	1	290	
16:38	6	6	4	1	0	2	291	
16:39	6	8	0	5	0	2	282	
16:40	3	4	6	3	0	0	284	
16:41	7	7	6	2	0	0	288	
16:42	5	2	11	5	0	0	297	
16:43	4	6	7	5	0	0	297	
16:44	4	9	6	8	0	0	290	
16:45	7	4	0	5	1	0	289	
16:46	5	8	6	8	0	0	284	
16:47	6	4	6	4	1	0	289	
16:48	7	4	0	4	1	1	288	
16:49	3	8	2	5	2	0	282	
16:50	10	4	8	11	1	0	286	
16:51	12	0	5	7	0	0	296	
16:52	9	4	5	3	0	0	303	
16:53	10	6	2	11	0	2	296	
16:54	5	10	4	7	0	2	286	
16:55	3	11	6	3	0	0	281	
16:56	4	4	4	8	0	0	277	
16:57	3	1	3	2	0	2	278	
16:58	5	10	1	0	2	1	275	
16:59	5	3	11	7	3	0	284	
17:00	3	4	3	8	0	0	278	
17:01	3	7	2	2	2	0	276	
17:02	5	7	2	1	0	0	275	
17:03	2	12	6	8	0	0	263	
17:04	8	7	8	5	0	1	266	
17:05	9	12	15	5	2	0	275	
17:06	10	8	3	5	0	0	275	
17:07	12	10	3	0	0	1	279	
17:08	13	4	1	8	0	0	281	
17:09	2	11	11	12	1	0	272	
17:10	2	3	1	3	0	0	269	
17:11	9	1	7	4	2	0	282	
17:12	1	1	4	2	0	0	284	
17:13	6	4	1	2	0	0	285	
17:14	2	9	4	4	0	0	278	
17:15	10	3	10	6	2	0	291	
17:16	1	5	3	12	0	0	278	
17:17	6	6	12	5	0	0	285	
17:18	0	10	3	6	0	0	272	
17:19	5	10	5	5	0	0	267	
17:20	7	8	3	8	0	0	261	
17:21	15	6	7	9	1	1	268	

Walmart Supercenter 1333 North Mountain Ave Ontario, CA 91762	Market Area Enter	Market Area Exit	Pharmacy Area Enter	Pharmacy Area Exit	Garden Area Enter	Garden Area Exit	Running Total
17:22	10	7	3	4	0	0	270
17:23	7	1	8	7	0	0	277
17:24	3	7	10	12	0	0	271
17:25	8	6	0	2	0	0	271
17:26	8	4	4	9	5	0	275
17:27	4	7	5	6	1	0	272
17:28	6	3	5	9	2	2	271
17:29	6	3	3	3	0	0	274
17:30	2	6	5	1	0	0	274
17:31	7	3	9	9	0	3	275
17:32	3	8	8	1	0	0	277
17:33	5	5	5	0	0	0	282
17:34	7	5	1	10	1	2	274
17:35	2	11	1	11	0	0	255
17:36	6	9	7	10	0	0	249
17:37	1	10	2	3	0	0	239
17:38	12	4	9	6	1	0	251
17:39	7	4	10	1	0	0	263
17:40	10	9	12	3	0	0	273
17:41	0	0	13	2	0	0	284
17:42	9	5	6	5	3	0	292
17:43	9	8	6	6	0	0	293
17:44	4	3	5	1	0	0	298
17:45	10	6	8	1	0	0	309
17:46	6	7	1	6	2	0	305
17:47	6	5	6	2	0	0	310
17:48	6	5	3	5	0	0	309
17:49	4	8	4	7	0	0	302
17:50	8	11	2	7	0	0	294
17:51	3	5	4	4	0	0	292
17:52	8	10	4	4	0	0	290
17:53	5	7	2	2	0	0	288
17:54	2	5	4	0	0	2	287
17:55	6	4	10	5	0	0	294
17:56	2	3	6	5	0	0	294
17:57	6	4	4	16	0	0	284
17:58	0	3	6	4	0	0	283
17:59	3	7	8	5	0	0	282
Total	686	709	610	629	63	47	

Walmart Supercenter 1333 North Mountain Ave Ontario, CA 91762	Market Area Enter	Market Area Exit	Pharmacy Area Enter	Pharmacy Area Exit	Garden Area Enter	Garden Area Exit	Running Total
Starting Occupancy							13
7:00	0	0	0	0	0	0	13
7:01	0	0	0	0	0	0	13
7:02	0	0	0	0	0	0	13
7:03	0	0	0	0	0	0	13
7:04	0	0	0	0	0	0	13
7:05	2	2	0	0	0	0	13
7:06	0	2	1	0	0	0	12
7:07	1	2	1	0	0	0	12
7:08	0	0	0	0	0	0	12
7:09	2	1	0	0	0	0	13
7:10	1	2	0	1	0	0	11
7:11	3	2	0	0	0	0	12
7:12	0	1	0	0	0	0	11
7:13	0	0	0	0	0	0	11
7:14	1	0	0	0	0	0	12
7:15	1	0	0	0	0	0	13
7:16	2	4	0	0	0	0	11
7:17	4	0	0	0	0	0	15
7:18	0	0	0	0	0	0	15
7:19	0	0	0	0	0	0	15
7:20	1	0	0	0	0	0	16
7:21	0	3	0	0	0	0	13
7:22	0	0	0	0	0	0	13
7:23	4	2	1	0	0	0	16
7:24	2	3	1	0	0	0	16
7:25	0	2	0	0	0	0	14
7:26	2	1	0	0	0	0	15
7:27	1	0	1	0	0	0	17
7:28	0	1	0	0	0	0	16
7:29	0	1	0	1	0	0	14
7:30	0	0	3	0	0	0	17
7:31	3	2	0	0	0	0	18
7:32	4	1	0	0	0	0	21
7:33	4	0	0	0	0	0	25
7:34	2	1	0	1	0	0	25
7:35	0	2	4	2	0	0	25
7:36	2	0	0	0	0	0	27
7:37	0	2	1	1	0	0	25
7:38	1	0	0	0	0	0	26
7:39	1	0	0	0	0	0	27
7:40	1	0	0	0	0	0	28
7:41	0	2	1	0	0	0	27
7:42	1	1	0	0	0	0	27
7:43	0	1	0	0	0	0	26
7:44	0	2	1	0	0	0	25
7:45	5	1	0	2	0	0	27
7:46	3	2	1	0	0	0	29
7:47	0	0	2	1	0	0	30
7:48	1	0	1	1	0	0	31
7:49	1	2	0	0	0	0	30
7:50	3	1	0	0	0	0	32
7:51	1	3	1	1	0	0	30
7:52	1	0	1	0	0	0	32
7:53	1	0	3	0	0	0	36
7:54	2	0	0	1	0	0	37
7:55	3	0	0	0	0	0	40
7:56	5	1	1	0	0	0	45
7:57	1	0	5	0	0	0	51
7:58	4	1	2	1	0	0	55
7:59	1	0	3	0	0	0	59
8:00	1	2	0	2	0	0	56
8:01	2	0	1	1	0	0	58
8:02	3	2	1	0	0	0	60
8:03	0	1	2	0	0	0	61
8:04	3	3	1	0	0	0	62
8:05	1	1	1	2	0	0	61
8:06	2	1	1	1	0	0	62
8:07	6	0	2	0	0	0	70
8:08	3	2	0	2	0	0	69
8:09	2	3	1	0	0	0	69
8:10	2	0	1	0	1	0	73
8:11	2	0	3	0	2	0	80
8:12	1	5	0	1	1	0	76
8:13	2	3	4	0	0	0	79
8:14	3	2	1	0	0	0	81
8:15	2	2	1	3	0	0	79
8:16	5	2	1	0	0	0	83
8:17	2	1	2	0	0	0	86
8:18	3	1	0	0	0	0	88
8:19	3	0	2	2	0	0	91
8:20	0	3	0	0	0	0	88
8:21	2	1	3	0	0	0	92

Walmart Supercenter 1333 North Mountain Ave Ontario, CA 91762	Market Area Enter	Market Area Exit	Pharmacy Area Enter	Pharmacy Area Exit	Garden Area Enter	Garden Area Exit	Running Total
8:22	0	1	0	0	0	0	91
8:23	6	2	0	0	0	0	95
8:24	0	0	5	1	0	0	99
8:25	1	1	1	0	0	0	100
8:26	2	1	1	0	0	0	102
8:27	3	1	3	1	0	0	106
8:28	1	2	2	3	0	0	104
8:29	2	2	0	1	0	0	103
8:30	1	0	1	0	0	0	105
8:31	2	3	0	1	0	0	103
8:32	1	2	2	1	0	0	103
8:33	0	1	4	1	0	0	105
8:34	1	1	3	2	0	1	105
8:35	3	3	4	6	0	0	103
8:36	2	4	4	2	0	0	103
8:37	5	0	0	3	0	0	105
8:38	3	4	2	1	0	0	105
8:39	4	1	6	1	0	0	113
8:40	1	3	0	0	0	0	111
8:41	3	2	4	1	1	0	116
8:42	0	2	0	0	0	0	114
8:43	1	0	1	1	0	0	115
8:44	0	2	2	2	0	0	113
8:45	1	1	1	0	0	0	114
8:46	0	3	3	2	0	0	112
8:47	1	4	0	1	0	0	108
8:48	2	5	0	0	0	0	105
8:49	1	2	2	2	0	0	104
8:50	3	2	2	1	0	0	106
8:51	0	0	2	1	0	0	107
8:52	1	1	4	2	0	0	109
8:53	0	2	1	0	0	0	108
8:54	4	1	2	0	0	0	113
8:55	7	4	5	0	0	0	121
8:56	3	3	2	1	0	0	122
8:57	1	1	7	0	0	0	129
8:58	2	4	2	3	0	0	126
8:59	1	1	0	0	1	0	127
Total	197	161	141	68	6	1	

Walmart Supercenter 1333 North Mountain Ave Ontario, CA 91762	Market Area Enter	Market Area Exit	Pharmacy Area Enter	Pharmacy Area Exit	Garden Area Enter	Garden Area Exit	Running Total
Starting Occupancy							263
16:00	0	0	0	4	1	0	260
16:01	0	0	2	14	2	2	248
16:02	3	0	7	3	0	3	252
16:03	5	6	14	2	0	0	263
16:04	6	2	3	6	1	0	265
16:05	5	8	6	4	3	1	266
16:06	4	4	2	4	0	1	263
16:07	0	4	0	1	0	0	258
16:08	3	5	0	2	0	0	254
16:09	8	4	3	5	0	0	256
16:10	2	9	1	0	4	0	254
16:11	8	7	0	4	0	1	250
16:12	13	2	0	7	0	0	254
16:13	8	5	2	2	0	0	257
16:14	9	10	4	9	0	0	251
16:15	12	7	8	4	1	0	261
16:16	8	7	5	11	0	0	256
16:17	5	6	2	10	0	0	247
16:18	4	4	6	1	0	0	252
16:19	11	7	6	10	0	0	252
16:20	10	6	1	7	3	2	251
16:21	15	2	2	8	0	0	258
16:22	4	4	8	3	4	0	267
16:23	1	2	5	6	1	0	266
16:24	9	7	6	1	1	0	274
16:25	8	7	7	8	0	0	274
16:26	6	2	1	3	0	0	276
16:27	7	9	1	8	0	0	267
16:28	6	7	3	2	0	0	267
16:29	2	6	5	5	3	0	266
16:30	10	1	2	4	0	0	273
16:31	3	3	3	6	1	3	268
16:32	11	10	1	5	0	0	265
16:33	7	5	2	1	0	1	267
16:34	5	8	9	3	1	0	271
16:35	1	4	3	3	0	0	268
16:36	8	1	12	2	0	0	285
16:37	18	9	1	2	0	0	293
16:38	14	12	6	2	0	0	299
16:39	3	3	10	9	0	2	298
16:40	7	6	2	4	0	1	296
16:41	6	3	6	3	3	0	305
16:42	5	5	5	1	0	0	309
16:43	10	4	4	7	0	0	312
16:44	10	5	3	2	1	0	319
16:45	4	6	9	12	0	0	314
16:46	10	13	7	7	0	0	311
16:47	1	1	3	3	0	0	311
16:48	13	4	1	3	0	0	318
16:49	4	6	3	2	0	0	317
16:50	13	4	7	10	0	0	323
16:51	9	5	8	4	0	0	331
16:52	9	8	12	3	1	0	342
16:53	2	7	4	2	0	0	339
16:54	17	4	5	3	0	0	354
16:55	11	7	4	3	0	1	358
16:56	2	13	5	10	0	2	340
16:57	10	13	4	0	1	1	341
16:58	4	5	7	3	0	1	343
16:59	7	2	11	2	0	3	354
17:00	4	8	2	8	1	0	345
17:01	3	1	3	3	0	0	347
17:02	5	6	6	9	0	0	343
17:03	8	3	11	4	1	0	356
17:04	5	3	5	4	1	2	358
17:05	1	4	3	2	0	0	356
17:06	4	18	4	4	1	0	343
17:07	18	10	4	4	0	0	351
17:08	0	1	7	3	0	0	354
17:09	8	4	4	6	0	0	356
17:10	7	13	4	5	0	0	349
17:11	8	10	2	1	0	0	348
17:12	11	1	2	2	0	0	358
17:13	2	9	6	1	2	0	358
17:14	6	6	4	1	0	1	360
17:15	8	8	6	1	0	0	365
17:16	16	6	8	2	0	1	380
17:17	6	3	1	0	3	0	387
17:18	3	4	0	6	1	0	381
17:19	7	9	0	5	0	0	374
17:20	5	11	2	6	0	0	364
17:21	9	11	2	3	0	0	361

Walmart Supercenter 1333 North Mountain Ave Ontario, CA 91762	Market Area Enter	Market Area Exit	Pharmacy Area Enter	Pharmacy Area Exit	Garden Area Enter	Garden Area Exit	Running Total
17:22	5	4	5	4	0	0	363
17:23	4	7	6	3	0	0	363
17:24	1	16	9	2	0	0	355
17:25	6	7	11	0	0	1	364
17:26	3	3	2	15	0	0	351
17:27	9	3	4	7	0	0	354
17:28	4	5	5	11	1	2	346
17:29	5	2	11	6	0	0	354
17:30	6	9	3	8	0	0	346
17:31	6	5	4	7	0	0	344
17:32	12	8	1	2	0	0	347
17:33	4	7	2	5	0	0	341
17:34	7	1	3	5	0	0	345
17:35	9	8	3	5	0	0	344
17:36	8	18	6	8	1	0	333
17:37	4	3	5	5	0	0	334
17:38	4	4	5	9	0	0	330
17:39	5	9	1	4	0	0	323
17:40	15	3	4	7	1	0	333
17:41	5	7	5	4	0	0	332
17:42	3	7	9	8	0	0	329
17:43	16	6	3	4	0	0	338
17:44	1	6	0	7	0	0	326
17:45	17	13	3	8	1	0	326
17:46	6	4	9	4	0	0	333
17:47	5	6	5	9	0	0	328
17:48	5	4	1	13	0	0	317
17:49	5	6	1	2	0	0	315
17:50	10	13	3	8	2	0	309
17:51	5	3	2	4	0	0	309
17:52	3	5	3	8	0	0	302
17:53	15	2	5	4	0	0	316
17:54	11	3	11	3	0	0	332
17:55	14	11	7	6	0	2	334
17:56	11	3	5	4	0	0	343
17:57	2	5	5	9	0	0	336
17:58	7	3	1	6	0	0	335
17:59	2	5	0	7	1	0	326
Total	820	709	523	586	49	34	

Walmart Supercenter 1333 North Mountain Ave Ontario, CA 91762	Market Area Enter	Market Area Exit	Pharmacy Area Enter	Pharmacy Area Exit	Garden Area Enter	Garden Area Exit	Running Total
Starting Occupancy							248
13:00	0	0	0	0	0	0	248
13:01	0	0	4	10	0	0	242
13:02	0	0	1	9	3	1	236
13:03	7	3	3	5	2	0	240
13:04	1	4	8	1	0	0	244
13:05	2	3	4	5	3	0	245
13:06	4	2	2	2	0	0	247
13:07	4	7	5	6	0	0	243
13:08	3	8	7	5	0	0	240
13:09	5	6	7	8	1	0	239
13:10	1	4	5	2	3	0	242
13:11	4	0	4	4	1	0	247
13:12	5	6	4	9	0	0	241
13:13	2	10	1	1	0	0	233
13:14	3	2	1	4	0	0	231
13:15	4	8	6	3	1	1	230
13:16	3	7	1	5	1	1	222
13:17	4	3	6	5	0	0	224
13:18	0	3	2	11	0	1	211
13:19	2	6	10	2	2	1	216
13:20	3	4	3	7	0	2	209
13:21	13	1	4	5	0	0	220
13:22	1	4	5	5	0	0	217
13:23	3	0	3	2	0	1	220
13:24	7	5	3	6	0	0	219
13:25	5	3	6	3	2	0	226
13:26	4	3	6	4	0	0	229
13:27	5	2	5	4	0	0	233
13:28	1	3	1	2	0	0	230
13:29	5	4	8	0	0	0	239
13:30	7	7	2	2	0	0	239
13:31	10	9	3	3	2	0	242
13:32	8	5	3	7	0	0	241
13:33	1	2	5	4	0	1	240
13:34	3	4	3	8	0	1	233
13:35	3	7	7	2	0	0	234
13:36	5	1	7	0	1	0	246
13:37	4	4	4	5	1	0	246
13:38	9	1	3	1	0	0	256
13:39	2	0	8	9	2	0	259
13:40	9	8	3	1	0	0	262
13:41	6	6	2	4	2	1	261
13:42	0	5	8	4	0	0	260
13:43	2	7	6	6	0	0	255
13:44	9	2	4	8	0	1	257
13:45	4	0	5	4	0	1	261
13:46	1	1	1	4	1	0	259
13:47	9	4	4	9	1	1	259
13:48	1	3	8	3	0	0	262
13:49	2	4	1	6	0	0	255
13:50	8	6	7	5	0	0	259
13:51	0	3	2	3	2	0	257
13:52	6	10	2	0	0	0	255
13:53	3	5	9	9	0	0	253
13:54	1	7	5	1	0	1	250
13:55	3	4	7	9	0	0	247
13:56	2	6	5	7	0	3	238
13:57	1	2	2	0	2	0	241
13:58	4	0	5	9	1	2	240
13:59	1	15	4	3	0	3	224
14:00	3	5	2	4	1	0	221
14:01	6	3	5	5	1	2	223
14:02	12	5	3	5	2	0	230
14:03	4	5	5	0	2	1	235
14:04	2	6	1	5	2	0	229
14:05	3	7	2	6	0	1	220
14:06	2	6	5	8	0	0	213
14:07	2	2	3	0	0	0	216
14:08	6	8	1	10	0	0	205
14:09	6	1	0	1	1	0	210
14:10	9	4	1	10	1	0	207
14:11	2	6	1	7	0	0	197
14:12	9	7	8	4	3	0	206
14:13	12	6	2	3	0	0	211
14:14	2	5	4	6	2	0	208
14:15	2	2	1	2	2	2	207
14:16	7	2	7	3	0	0	216
14:17	2	6	2	7	0	0	207
14:18	2	4	2	5	0	0	202
14:19	2	2	4	2	0	0	204
14:20	8	0	4	6	0	0	210
14:21	4	6	5	6	0	0	207

Walmart Supercenter 1333 North Mountain Ave Ontario, CA 91762	Market Area Enter	Market Area Exit	Pharmacy Area Enter	Pharmacy Area Exit	Garden Area Enter	Garden Area Exit	Running Total
14:22	1	6	2	7	0	0	197
14:23	4	6	3	1	2	1	198
14:24	9	7	3	0	0	1	202
14:25	8	11	1	6	0	0	194
14:26	0	6	3	2	3	1	191
14:27	9	5	4	2	0	2	195
14:28	2	6	4	6	1	1	189
14:29	9	4	6	3	0	0	197
14:30	6	3	8	10	0	0	198
14:31	6	4	2	0	0	2	200
14:32	2	7	6	4	0	0	197
14:33	3	0	4	1	0	0	203
14:34	1	4	4	2	1	1	202
14:35	2	4	3	3	0	0	200
14:36	11	5	3	3	5	0	211
14:37	5	4	4	2	0	0	214
14:38	6	7	4	2	0	0	215
14:39	5	4	6	0	0	0	222
14:40	8	2	6	5	0	0	229
14:41	6	8	3	3	0	0	227
14:42	3	3	6	5	0	0	228
14:43	5	7	10	7	2	0	231
14:44	5	3	11	1	0	0	243
14:45	0	12	6	5	3	1	234
14:46	4	8	14	4	0	0	240
14:47	0	2	6	5	0	0	239
14:48	5	9	6	5	0	2	234
14:49	3	3	3	2	0	0	235
14:50	4	4	7	7	0	0	235
14:51	11	8	3	0	0	0	241
14:52	13	12	6	1	2	0	249
14:53	3	5	1	4	0	0	244
14:54	0	0	9	9	0	0	244
14:55	6	2	2	10	0	0	240
14:56	11	7	2	2	2	0	246
14:57	2	6	4	7	1	0	240
14:58	4	10	5	2	0	0	237
14:59	5	6	10	13	0	0	233
Total	519	557	518	527	73	41	



Walmart Supercenter 1333 North Mountain Ave Ontario, CA 91762	Market Area Enter	Market Area Exit	Pharmacy Area Enter	Pharmacy Area Exit	Garden Area Enter	Garden Area Exit	Running Total
Starting Occupancy							479
16:00	4	8	5	3	0	0	477
16:01	3	8	2	7	1	0	468
16:02	8	7	5	1	3	0	476
16:03	8	10	8	7	0	0	475
16:04	12	6	9	6	0	0	484
16:05	5	9	2	6	1	4	473
16:06	4	11	8	5	4	0	473
16:07	2	8	5	15	0	0	457
16:08	5	9	5	20	1	1	438
16:09	13	14	5	11	0	0	431
16:10	8	6	6	14	2	0	427
16:11	2	7	12	14	0	0	420
16:12	7	6	5	8	0	1	417
16:13	6	12	8	10	1	0	410
16:14	7	4	4	6	4	1	414
16:15	5	6	5	4	0	1	413
16:16	8	5	5	9	0	0	412
16:17	14	4	5	8	0	0	419
16:18	4	9	8	19	0	2	401
16:19	8	6	5	6	0	0	402
16:20	8	11	12	9	3	0	405
16:21	5	4	3	9	0	0	400
16:22	4	5	5	8	0	1	395
16:23	8	9	3	4	2	0	395
16:24	1	8	5	6	0	1	386
16:25	15	3	12	7	0	0	403
16:26	12	4	9	14	0	0	406
16:27	1	7	6	2	0	0	404
16:28	7	7	3	4	3	0	406
16:29	6	14	1	2	0	0	397
16:30	9	2	9	2	3	0	414
16:31	7	2	4	9	0	0	414
16:32	5	1	2	6	0	0	414
16:33	4	6	5	11	0	0	406
16:34	1	5	14	10	1	0	407
16:35	9	10	9	8	0	0	407
16:36	8	1	13	5	0	0	422
16:37	6	9	7	9	0	1	416
16:38	10	7	8	0	0	3	424
16:39	9	5	8	10	0	3	423
16:40	1	2	2	1	0	0	423
16:41	11	4	4	4	3	0	433
16:42	2	12	6	5	0	0	424
16:43	4	14	12	8	0	1	417
16:44	7	6	14	10	0	0	422
16:45	3	11	4	8	2	0	412
16:46	10	9	0	7	2	0	408
16:47	12	4	10	11	1	0	416
16:48	2	2	24	12	0	0	428
16:49	1	9	5	1	1	0	425
16:50	3	3	7	6	0	0	426
16:51	7	8	5	0	1	1	430
16:52	10	7	5	7	0	0	431
16:53	4	8	6	8	0	0	425
16:54	14	9	2	5	3	2	428
16:55	10	7	18	2	3	0	450
16:56	6	12	14	12	1	0	447
16:57	7	9	4	5	0	0	444
16:58	12	7	2	12	2	0	441
16:59	7	5	9	6	0	2	444
17:00	0	7	3	3	2	0	439
17:01	10	6	13	8	0	0	448
17:02	2	10	6	9	1	3	435
17:03	8	9	2	3	1	0	434
17:04	15	9	2	8	0	0	434
17:05	12	6	4	9	0	0	435
17:06	7	4	2	7	1	2	432
17:07	11	13	4	9	2	0	427
17:08	2	2	13	6	0	1	433
17:09	2	8	3	7	0	0	423
17:10	5	1	9	8	0	0	428
17:11	10	1	7	8	0	0	436
17:12	7	10	5	12	0	1	425
17:13	13	5	8	10	0	0	431
17:14	9	11	6	9	0	0	426
17:15	9	6	2	12	2	0	421
17:16	8	3	6	10	0	0	422
17:17	4	15	13	6	0	1	417
17:18	3	7	2	4	2	0	413
17:19	5	5	6	15	1	0	405
17:20	2	11	5	9	0	0	392
17:21	3	10	5	10	2	3	379

Walmart Supercenter 1333 North Mountain Ave Ontario, CA 91762	Market Area Enter	Market Area Exit	Pharmacy Area Enter	Pharmacy Area Exit	Garden Area Enter	Garden Area Exit	Running Total
17:22	9	3	8	3	0	2	388
17:23	12	18	6	10	0	0	378
17:24	6	4	7	6	4	1	384
17:25	11	8	6	13	1	0	381
17:26	14	5	3	9	2	2	384
17:27	6	9	24	8	0	1	396
17:28	14	3	2	11	0	1	397
17:29	12	7	7	8	0	0	401
17:30	0	8	2	14	2	0	383
17:31	7	8	4	13	1	0	374
17:32	2	3	1	1	2	0	375
17:33	11	5	9	5	3	0	388
17:34	4	6	1	9	0	0	378
17:35	13	3	9	3	0	0	394
17:36	8	6	7	4	0	0	399
17:37	0	13	7	8	0	0	385
17:38	6	8	3	12	2	0	376
17:39	1	11	6	10	1	0	363
17:40	1	10	18	6	0	2	364
17:41	9	1	7	9	0	1	369
17:42	3	5	2	6	1	0	364
17:43	3	5	8	6	0	0	364
17:44	12	2	12	10	0	0	376
17:45	0	9	8	1	0	0	374
17:46	5	2	3	9	1	2	370
17:47	5	10	15	7	0	0	373
17:48	0	5	9	8	0	0	369
17:49	11	10	15	6	3	0	382
17:50	6	9	10	5	0	0	384
17:51	10	2	0	3	0	0	389
17:52	3	2	8	1	1	2	396
17:53	12	9	3	0	4	0	406
17:54	3	3	13	10	0	4	405
17:55	0	2	13	10	0	1	405
17:56	3	0	6	11	2	1	404
17:57	3	10	6	9	0	0	394
17:58	9	6	1	8	0	0	390
17:59	2	6	6	4	3	0	391
Total	784	818	809	902	95	56	

Walmart Supercenter 1333 North Mountain Ave Ontario, CA 91762	Market Area Enter	Market Area Exit	Pharmacy Area Enter	Pharmacy Area Exit	Garden Area Enter	Garden Area Exit	Running Total
Starting Occupancy							501
13:00	8	5	8	7	0	0	505
13:01	0	9	4	5	0	0	495
13:02	14	4	4	5	4	0	508
13:03	8	0	13	7	0	1	521
13:04	18	3	0	6	2	0	532
13:05	6	1	6	7	6	0	542
13:06	6	8	2	1	2	0	543
13:07	2	8	14	5	0	4	542
13:08	7	5	4	4	0	8	536
13:09	1	12	3	4	0	0	524
13:10	9	14	4	10	0	2	511
13:11	4	7	7	6	0	0	509
13:12	10	6	6	6	0	0	513
13:13	12	3	0	10	0	0	512
13:14	5	5	10	6	4	0	520
13:15	2	9	9	9	0	1	512
13:16	7	6	4	5	0	0	512
13:17	10	10	6	6	0	0	512
13:18	6	4	7	9	0	0	512
13:19	8	4	10	15	0	1	510
13:20	6	3	6	9	0	0	510
13:21	4	11	1	7	3	3	497
13:22	4	11	6	6	0	0	490
13:23	6	9	2	3	7	0	493
13:24	8	7	10	7	0	3	494
13:25	1	23	3	6	1	1	469
13:26	13	6	4	13	0	0	467
13:27	3	7	15	10	2	2	468
13:28	0	10	1	1	0	0	458
13:29	5	3	4	9	0	3	452
13:30	6	5	3	9	2	0	449
13:31	5	9	15	6	3	0	457
13:32	11	4	5	10	0	2	457
13:33	3	7	5	4	4	0	458
13:34	10	7	5	10	0	2	454
13:35	8	9	11	5	0	2	457
13:36	11	9	4	8	0	4	451
13:37	2	5	5	3	8	0	458
13:38	9	4	5	7	0	4	457
13:39	3	7	12	4	5	0	466
13:40	6	4	5	15	0	3	455
13:41	5	5	3	4	9	2	461
13:42	8	5	3	7	1	0	461
13:43	1	3	3	7	1	2	454
13:44	0	4	7	7	0	0	450
13:45	2	5	9	7	0	0	449
13:46	9	13	3	9	2	0	441
13:47	2	15	7	12	0	2	421
13:48	3	9	17	19	0	0	413
13:49	9	0	15	9	3	0	431
13:50	4	7	5	10	0	0	423
13:51	11	2	3	4	7	3	435
13:52	1	6	12	8	3	0	437
13:53	7	4	10	8	1	0	443
13:54	5	7	13	10	1	0	445
13:55	4	4	5	2	0	2	446
13:56	7	6	12	6	2	6	449
13:57	11	8	5	8	4	2	451
13:58	14	6	14	5	4	0	472
13:59	7	4	3	2	0	0	476
14:00	13	13	4	8	0	0	472
14:01	11	1	2	20	0	0	464
14:02	17	6	9	2	0	4	478
14:03	5	4	11	10	0	0	480
14:04	8	7	8	3	2	2	486
14:05	5	7	14	7	2	1	492
14:06	1	5	11	15	4	2	486
14:07	5	4	7	6	5	0	493
14:08	7	3	6	9	2	0	496
14:09	9	4	0	1	4	0	504
14:10	4	6	8	7	9	0	512
14:11	5	2	8	8	2	5	512
14:12	10	6	6	8	3	0	517
14:13	4	9	6	9	2	0	511
14:14	14	2	7	6	0	0	524
14:15	19	4	6	5	2	0	542
14:16	10	4	11	8	0	0	551
14:17	8	14	11	7	2	1	550
14:18	11	7	6	5	5	0	560
14:19	11	7	9	5	2	0	570
14:20	5	6	13	2	2	0	582
14:21	2	9	8	23	6	3	563

Walmart Supercenter 1333 North Mountain Ave Ontario, CA 91762	Market Area Enter	Market Area Exit	Pharmacy Area Enter	Pharmacy Area Exit	Garden Area Enter	Garden Area Exit	Running Total
14:22	2	5	13	15	0	0	558
14:23	1	9	10	11	0	0	549
14:24	11	2	7	12	6	2	557
14:25	4	10	10	11	0	1	549
14:26	10	5	6	5	0	2	553
14:27	7	3	4	3	0	0	558
14:28	8	6	10	3	0	0	567
14:29	4	17	12	5	2	0	563
14:30	19	3	5	17	2	2	567
14:31	7	9	3	11	1	0	558
14:32	15	5	10	15	1	2	562
14:33	7	6	4	9	0	0	558
14:34	9	8	5	22	2	2	542
14:35	14	8	11	8	0	2	549
14:36	2	4	4	17	2	0	536
14:37	11	22	8	9	0	0	524
14:38	13	17	7	9	2	0	520
14:39	9	7	3	6	0	0	519
14:40	11	12	0	15	0	0	503
14:41	11	5	17	11	0	2	513
14:42	1	10	4	11	0	0	497
14:43	7	10	3	8	4	0	493
14:44	9	17	3	8	2	0	482
14:45	7	10	17	6	0	0	490
14:46	5	12	5	11	0	0	477
14:47	3	4	1	3	0	5	469
14:48	14	8	8	11	0	0	472
14:49	4	12	6	11	2	0	461
14:50	4	11	3	11	2	0	448
14:51	4	5	5	2	0	0	450
14:52	6	3	4	7	0	3	447
14:53	8	9	4	5	0	0	445
14:54	7	4	6	5	9	0	458
14:55	4	5	5	5	0	0	457
14:56	5	8	4	4	0	0	454
14:57	5	4	1	5	4	1	454
14:58	7	7	11	5	4	3	461
14:59	7	3	4	8	0	0	461
Total	843	831	811	943	190	110	

February 23, 2015

**RE: AIRPORT LAND USE COMMISSION  
WALMART 3129  
EASTVALE, CA  
P+R PROJECT NO. 11192**

To Whom It May Concern:

The risk-reduction design features that will be incorporated into the proposed Walmart building are as follows:

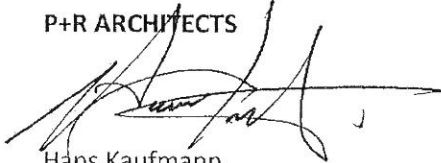
- **Single-story construction:** The proposed Walmart building will be a one-story structure.
- **Concrete Masonry construction.** The exterior walls of the proposed Walmart building will be primarily constructed using 8" x 8" x 16" std. concrete masonry units. Minimum code required masonry compressive strength of CMU is 1500 psi, however Walmart requires a minimum of 2000 psi for their projects. The CMU will have both vertical and horizontal reinforcing steel bars set and wired within the CMU cells spaced as required by the project structural engineer and building code requirements. Each CMU cell will be filled with concrete grout mix designed to a min. compressive strength of 2000 PSI. This assembly meets the requirements for a four-hour rated exterior wall system.
- **Upgraded strength of building roof.** The roof will be comprised of structural steel decks, steel trusses, steel beams, and steel columns which provides exceptional roof strength. The roof will be designed for a dead load of 15 psf, which is above the minimum requirement of 12 psf, and will be designed for the full 20 psf live load required by code without any live load reductions. The roof structure will also be designed for additional ponding loads around the roof drains to account for ponded water during heavy rain events. Although the proposed Walmart building will be utilizing skylights, the steel deck will be engineered to compensate for any deck area removed by the skylight openings. In addition, the skylights utilized are designed to carry 20 psf of the tributary roof loads and are tested to withstand a minimum of a 200 pound load dropped from 24" above center. Furthermore, skylights are distributed evenly across the building roof, as opposed to being clustered in specific areas which could degrade building strength. (See Table Below).

- **Limited use of windows.** The only windows to be utilized in the proposed Walmart building will face north and are located at the entrances to the store adjacent to the glass entrance doors. The total area of glass (including doors) will account for approximately only 11.6% of the north building face. The main focus of glass is at the entry points which includes both vestibules and which are roughly 27'-4" wide by 25'-4" high. The vestibules consist of several glass panels and engineered storefront framing. There are a total of 8 low windows which are 6'-8" high by 6'-0" wide. No windows or glass will be utilized on the east, west, and south elevations.
- **Enhanced fire sprinkler system.** The entire proposed Walmart building will be fully sprinklered with sprinkler densities that significantly exceed Code requirements for a mercantile occupancy. Working-level fire sprinkler plans, signed-and-sealed by a fire protection engineer licensed in California will be submitted at the time of construction plan submittal. Walmart uses an "owner-elected" density for the sales floor and stock room areas that exceed NFPA 13-specified densities by at least 21%. In addition, the fire sprinkler systems will include a minimum 5 psi safety factor, exceeding NFPA 13 requirements. (See Table Below).
- **Increased number of emergency exits.** The proposed Walmart building will exceed both the number of exits required by Code and the exiting width required by Code. The building will have at least two additional exits over what is required by Code and the required overall exit width will exceed at least 5 feet distributed among all of the various emergency exits. Emergency exits will be located on all four sides of the building and will be spaced to accommodate a quick exit from all areas of the building in the event of an emergency. Skylights will not be installed in the immediate vicinity of any of the emergency exits given that the placement of skylights is designed to allow natural light into the center of the store as opposed to the perimeter of the store where all emergency exits will be located. (See Table Below).
- **Additional project design feature.** The proposed Walmart building will utilize steel girders and joists which provides greater strength than a wood panelized system. (See Table Below).

Risk-Reduction Design Component	Exceedance
Upgraded Strength of Building Roof	The roof will be designed for a dead load of 15 psf, which is above the minimum requirement of 12 psf
Enhanced Fire Sprinkler System	Will exceed NFPA 13-specified densities by at least 21%. In addition, the fire sprinkler system will include a minimum 5 psi safety factor, exceeding NFPA 13 requirements.
Number of Emergency Exits	At least two additional emergency exits over what is required by code
Emergency Exit Width	Overall exit width will exceed at least 5 feet distributed among all of the various emergency exits
Additional Project Design Feature	The proposed Walmart building will utilize steel girders and joists which provides greater strength than a wood panelized system; a wood panelized system is allowed to be utilized but steel is being used as opposed to wood.

Sincerely,

P+R ARCHITECTS



Hans Kaufmann  
Senior Associate



702 SW 8<sup>th</sup> Street  
Bentonville, AR 72716-0500  
Phone 479.204.8820  
Fax 479.204.8964

February 24, 2015

Riverside County  
Airport Land Use Commission  
Riverside County Administrative Center  
4080 Lemon Street, 14th Floor  
Riverside, CA 92501

To Whom It May Concern:

Walmart has conducted an assessment of the maximum number of employees that would be working at normal peak times at the proposed Eastvale Walmart store and has concluded that 165 would be the maximum number of employees. It should be noted, however, that not every employee would be inside the building at a given time because some tasks are performed outside the building, such as collecting shopping carts. Moreover, employees are free to take rest breaks and meal breaks outside the building.

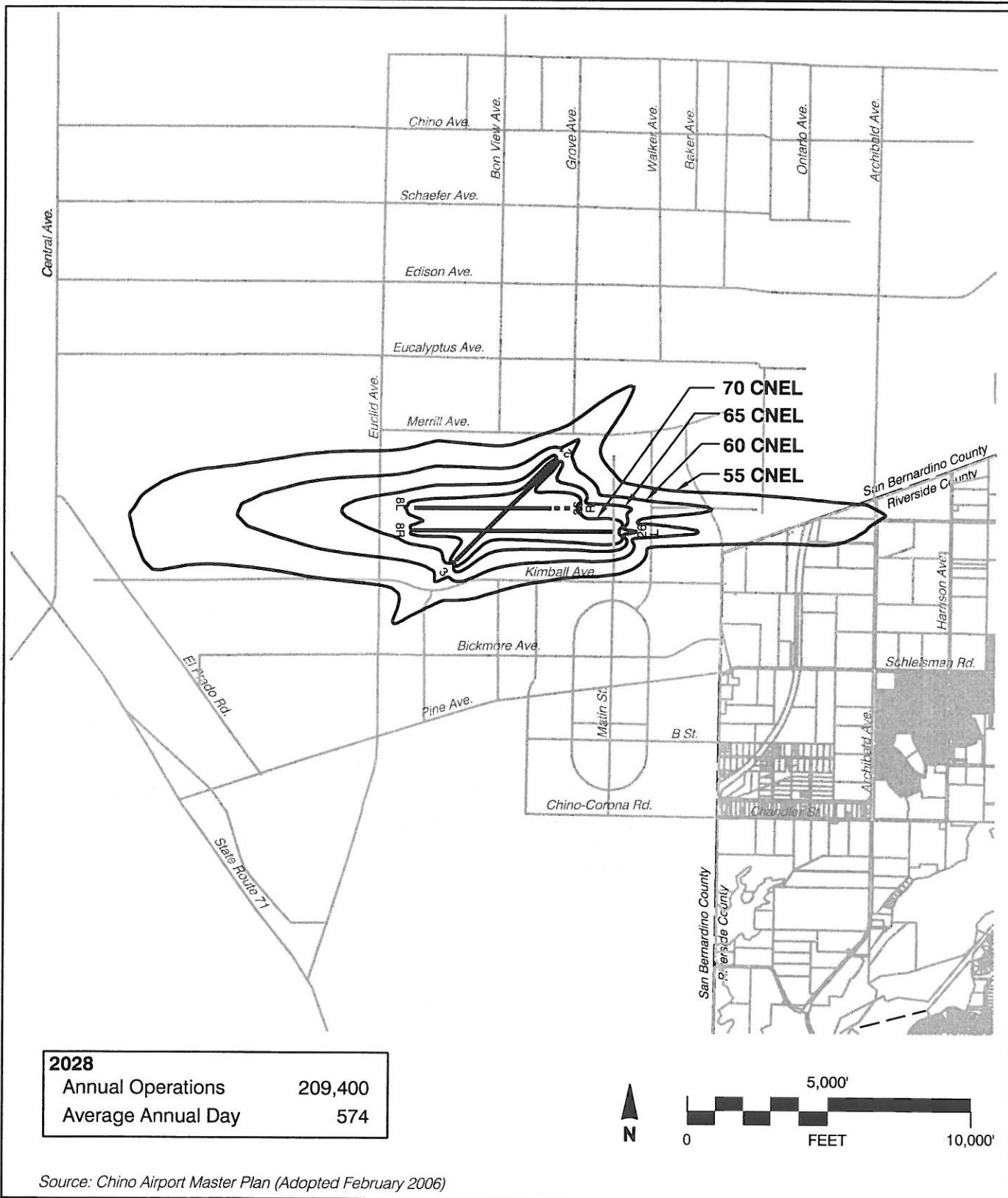
If you need any information, please do not hesitate to contact me.

Sincerely,

A handwritten signature in black ink, appearing to read "Jeff Doss", is written over a faint, larger version of the same signature.

Jeff Doss  
Sr. Manager of Project Management and Design





X:\18190-10\08001\TECH\Cadd\RCO\CNO\CNO-noise-compatibility-Oct.08.dwg Dec 01, 2008 - 4:43pm

**Map CH-3**

**Future Noise Impacts**  
**Chino Airport**

- 2.3 *Maximum Average Residential Lot Size in Compatibility Zone D Areas and Consistency of the County's Medium Density Residential Designation:* The Medium Density Residential designation shall be considered substantially consistent with the "higher intensity option" for Compatibility Zone D, provided that it is not implemented through zoning which would require a minimum net residential lot size greater than 0.2 acre. Projects in Compatibility Zone D shall be considered to be "substantially consistent" with the "higher intensity option" for Compatibility Zone D if the average size of residential lots (excluding lots utilized as common areas, public facilities, recreational areas, drainage basins, and open space) – either the mean or median – is 8,712 square feet (0.2 acre) or less in area.
- 2.4 *Nonresidential Intensity in Compatibility Zone B1:* An average of 40 people per acre shall be allowed on a site and up to 80 people shall be allowed to occupy any single acre of the site.
- 2.5 *Compatibility Zone D Rural Lifestyle Neighborhood Residential Densities:* The criteria set forth in Countywide Policy 3.1.3(a) and the Basic Compatibility Criteria matrix (Table 2A) notwithstanding, residential densities greater than or equal to 1.0 dwelling units per acre, but less than or equal to 2.0 dwelling units per acre, may be permitted in those portions of Compatibility Zone D located not more than one-half mile northerly of Chandler Street and westerly of Archibald Avenue.
- 2.6 *Compatibility Zone D Non-residential Intensities:* The criteria set forth in Countywide Policies 3.1.1, 3.1.4, and 4.2.5(b)(5) and the Basic Compatibility Criteria matrix (Table 2A) notwithstanding, the following usage intensity criteria shall apply within Compatibility Zone D: An average of 150 people per acre shall be allowed on a site and up to 450 people shall be allowed to occupy any single acre of the site.
- 2.7 *Calculation of Concentration of People:* The provisions of Table C1 in Appendix C notwithstanding, retail sales and display areas or "showrooms" (excluding restaurants and other uses specifically identified separately from retail in Table C1), shall be evaluated as having an intensity in persons per square foot of one person per 115 square feet of gross floor area without eligibility for the 50 percent reduction.

## Methods for Determining Concentrations of People

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One criterion used in the *Riverside County Airport Land Use Compatibility Plan* is the maximum number of people per acre that can be present in a given area at any one time. If a proposed use exceeds the maximum density, it is considered inconsistent with compatibility planning policies. This appendix provides some guidance on how the people-per-acre determination can be made.

The most difficult part about making a people-per-acre determination is estimating the number of people likely to use a particular facility. There are several methods which can be utilized, depending upon the nature of the proposed use:

- ▶ **Parking Ordinance**—The number of people present in a given area can be calculated based upon the number of parking spaces provided. Some assumption regarding the number of people per vehicle needs to be developed to calculate the number of people on-site. The number of people per acre can then be calculated by dividing the number of people on-site by the size of the parcel in acres. This approach is appropriate where the use is expected to be dependent upon access by vehicles. Depending upon the specific assumptions utilized, this methodology typically results in a number in the low end of the likely intensity for a given land use.
- ▶ **Maximum Occupancy**—The Uniform or California Building Code can be used as a standard for determining the maximum occupancy of certain uses. The chart provided as Table C1 indicates the required number of square feet per occupant. The number of people on the site can be calculated by dividing the total floor area of a proposed use by the minimum square feet per occupant requirement listed in the table. The maximum occupancy can then be divided by the size of the parcel in acres to determine the people per acre. Surveys of actual occupancy levels conducted by various agencies have indicated that many retail and office uses are generally occupied at no more than 50% of their maximum occupancy levels, even at the busiest times of day. Therefore, the number of people calculated for office and retail uses should usually be adjusted (50%) to reflect the actual occupancy levels before making the final people per acre determination. Even with this adjustment, the UBC-based methodology typically produces intensities at the high end of the likely range.
- ▶ **Survey of Similar Uses**—Certain uses may require an estimate based upon a survey of similar uses. This approach is more difficult, but is appropriate for uses which because of the nature of the use, cannot be reasonably estimated based upon parking or square footage.

Table C2 shows sample calculations.

<u>Use</u>	<u>Minimum Square Feet per Occupant</u>
1. Aircraft Hangars (no repair)	500
2. Auction Rooms	7
3. Assembly Areas, Concentrated Use (without fixed seats)	7
Auditoriums	
Churches and Chapels	
Dance Floors	
Lobby Accessory to Assembly Occupancy	
Lodge Rooms	
Reviewing Stands	
Stadiums	
Waiting Areas	3
4. Assembly Areas, Less Concentrated Use	15
Conference Rooms	
Dining Rooms	
Drinking Establishments	
Exhibit Rooms	
Gymnasiums	
Lounges	
Stages	
Gaming	11
5. Bowling Alley (assume no occupant load for bowling lanes)	4
6. Children's Homes and Homes for the Aged	80
7. Classrooms	20
8. Congregate Residences	200
9. Courtrooms	40
10. Dormitories	50
11. Dwellings	300
12. Exercising Rooms	50
13. Garage, Parking	200
14. Health-Care Facilities	80
Sleeping Rooms	120
Treatment Rooms	240
15. Hotels and Apartments	200
16. Kitchen – Commercial	200
17. Library Reading Room	50
Stack Areas	100
18. Locker Rooms	50
19. Malls	Varies
20. Manufacturing Areas	200
21. Mechanical Equipment Room	300
22. Nurseries for Children (Daycare)	35
23. Offices	100
24. School Shops and Vocational Rooms	50
25. Skating Rinks	50 on the skating area; 15 on the deck
26. Storage and Stock Rooms	300
27. Stores — Retail Sales Rooms	
Basements and Ground Floors	30
Upper Floors	60
28. Swimming Pools	50 for the pool area; 15 on the deck
29. Warehouses	500
30. All Others	100

Source: California Building Code (1998), Table 10-A

Table C1

## Occupancy Levels—California Building Code

# NOTICE OF PUBLIC HEARING

## RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14<sup>th</sup> Floor, Riverside, California 92501, Monday through Thursday, from 8:00 a.m. to 5:00 p.m., and by prescheduled appointment on Fridays, from 9:00 a.m. to 5:00 p.m.

PLACE OF HEARING: Riverside County Administration Center  
4080 Lemon St., 1<sup>st</sup> Floor Hearing Room  
Riverside, California

DATE OF HEARING: April 9, 2015

TIME OF HEARING: 9:00 A.M.

### CASE DESCRIPTION:

ZAP1020CH15 – Wal-Mart Stores, Inc. (Representatives: Kathleen Bonesz, Kimley-Horn and Associates; Jonathan Shardlow, Gresham Savage Nolan and Tilden) – City of Eastvale Case Number 12-0051 (Major Development Plan/Conditional Use Permit, General Plan Amendment [GPA], Change of Zone [CZ], Tentative Tract Map). The applicant proposes to develop Eastvale Crossings, a 218,100 square foot shopping center featuring a 192,000 square foot Wal-Mart, on 24.8 gross acres located at the southeast corner of Archibald Avenue and Limonite Avenue, northerly and westerly of Southern California Edison transmission lines and James C. Huber Park. The shopping center will also include two retail buildings with a combined floor area of 18,400 square feet, a 3,500 square foot fast food restaurant with drive-thru, and a gas station with a 4,200 square foot convenience store, 16 fueling positions, and car wash. The site is presently split between areas designated Light Industrial and Commercial Retail on the City's General Plan. The GPA would designate the entire site as Commercial Retail. Similarly, the site is presently split between areas zoned C-1/C-P (General Commercial) and M-SC (Manufacturing-Service Commercial). The CZ would apply C-1/C-P zoning to the entire site. Finally, Tentative Tract Map No. 35061 would divide the property into six (6) parcels. (Compatibility Zones C and D of the Chino Airport Influence Area).

FURTHER INFORMATION: Contact John Guerin at (951) 955-0982. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Mr. Eric Norris of the City of Eastvale Planning Department, at (951) 361-0900.

# APPLICATION FOR MAJOR LAND USE ACTION REVIEW

## RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

ALUC Identification No. \_\_\_\_\_

**PROJECT PROPONENT** (TO BE COMPLETED BY APPLICANT)

Date of Application February 25, 2015  
 Property Owner Wal-Mart Stores Real Estate Business Trust Phone Number (479) 273-4000  
 Mailing Address 2001 Southeast 10th Street, Bentonville, AR 72716-5510

Agent (if any) Kimley-Horn and Associates, Inc. Phone Number (714) 939-1030  
 Mailing Address 765 The City Drive, Orange, CA 92868

**PROJECT LOCATION** (TO BE COMPLETED BY APPLICANT)

*Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways*

Street Address Southeast Corner of Archibald Avenue and Limonite Avenue, Eastvale, CA  
 Assessor's Parcel No. 144-030-012-014-028 - See attached ALTA Survey Parcel Size 24.78 AC  
 Subdivision Name \_\_\_\_\_ (1.52 AC DEDICATED R/W)  
 Lot Number \_\_\_\_\_ Zoning Classification C1/CP

**PROJECT DESCRIPTION** (TO BE COMPLETED BY APPLICANT)

*If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed*

Existing Land Use (describe) Agricultural - See Attached ALTA Survey

Proposed Land Use (describe) Commercial Shopping Center - See Attached Site Plan

For Residential Uses Number of Parcels or Units on Site (exclude secondary units) N/A  
 For Other Land Uses Hours of Use 24 hours  
 (See Appendix C) Number of People on Site Maximum Number 1897 (218,100 sf / 115 sf occupant)  
 Method of Calculation California Building Code (CBC)  
See also Survey of Similar Uses

Height Data Height above Ground or Tallest Object (including antennas and trees) 32 feet ft.  
 Highest Elevation (above sea level) of Any Object or Terrain on Site 676 feet above sea level ft.

Flight Hazards Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?  Yes  No

If yes, describe \_\_\_\_\_  
 \_\_\_\_\_  
 \_\_\_\_\_

REFERRING AGENCY (APPLICANT OR JURISDICTION TO COMPLETE)		
Date Received	February 25, 2015	Type of Project
Agency Name	City of Eastvale	<input checked="" type="checkbox"/> General Plan Amendment
Staff Contact	Eric Norris	<input checked="" type="checkbox"/> Zoning Amendment or Variance
Phone Number	951-361-0900	<input checked="" type="checkbox"/> Subdivision Approval
Agency's Project No.	PP12-0051	<input checked="" type="checkbox"/> Use Permit
		<input type="checkbox"/> Public Facility
		<input type="checkbox"/> Other

**A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

**B. SUBMISSION PACKAGE:**

**ALUC REVIEW**

**STAFF REVIEW (Consult with ALUC staff planner as to whether project qualifies)**

- 1 . . . . . Completed Application Form
- 1 . . . . . Project Site Plan – Folded (8-1/2 x 14 max.)
- 1 . . . . . Elevations of Buildings - Folded
- 1 Each . 8 ½ x 11 reduced copy of the above
- 1 . . . . . 8 ½ x 11 reduced copy showing project in relationship to airport.
- 1 Set . Floor plans for non-residential projects
- 4 Sets . . Gummed address labels of the Owner and representative (**See Proponent**).
- 1 Set . . Gummed address labels of all property owners within a 300' radius of the project site. If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10), with ALUC return address.
- 4 Sets . . Gummed address labels of the referring agency (City or County).
- 1 . . . . . Check for Fee (See Item "C" below)

- 1 . . . . . Completed Application Form
- 1 . . . . . Project Site Plans – Folded (8-1/2 x 14 max.)
- 1 . . . . . Elevations of Buildings - Folded
- 1 . . . . . 8 ½ x 11 Vicinity Map
- 1 Set . Gummed address labels of the Owner and representative (**See Proponent**).
- 1 Set . Gummed address labels of the referring agency.
- 1 . . . . . Check for review—See Below

# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

## STAFF REPORT

### ADMINISTRATIVE ITEMS

- 4.1 Director's Approvals. As authorized pursuant to Section 1.5.2(d) of the 2004 Riverside County Airport Land Use Compatibility Plan, during the first half of the month of March, ALUC Director Ed Cooper reviewed three non-legislative cases and issued determinations of consistency.

ZAP1060FV15 pertains to a Development Plan application with the City of Murrieta proposing development of 196 apartment units in nine buildings up to three stories in height, along with clubhouse and pool, on 11.07 gross acres (9.85 acres) located southerly of Murrieta Hot Springs Road, easterly of its intersection with Via Princessa, within Airport Compatibility Zone D of the French Valley Airport Influence Area. The project's open area requirement would be primarily met within the south half of Murrieta Hot Springs Road. Use of the clubhouse and pool would be limited to residents and their guests. The site's elevation is considerably lower than the runway elevation.

ZAP1110MA15 pertains to a Revised Tentative Tract Map and Plot Plan application with the City of Moreno Valley proposing development of 273 apartment units in twenty buildings up to three stories in height, along with a leasing office, recreation center, and pool, on 13.9 gross acres (12.06 net acres) located southerly of Box Springs Road, easterly of the southerly extension of Clark Street, and northerly of State Highway Route 60, within Airport Compatibility Zone D of the March Air Reserve Base/Inland Port Airport Influence Area. While the top point elevation would be 100 feet higher than the runway elevation, the site's distance from the runway (17,900 feet) indicated that there was not a need for Federal Aviation Administration obstruction evaluation. (Structure height above ground level would not exceed 38.5 feet.)

ZAP1026PS15 pertains to a revised Building Permit application with the City of Palm Springs proposing development of a fitness center associated with the existing stroke recovery center on 4.74 acres located northerly of Alejo Road and easterly of Commercial Road, within Airport Compatibility Zone E of the Palm Springs Airport Influence Area. The property is located partially within Compatibility Zone B2, but the portion of the site where the building is proposed is located within Zone E. This project had been previously determined consistent, but was required to be reconsidered due to an increase in building height from 11.5 feet to 13.05 feet. FAA Obstruction Evaluation Service issued a Determination of No Hazard to Air Navigation for the building at its increased height and top point elevation on February 23, 2015.

Copies of the consistency letters and background documents are attached, for the Commission's information.

Y:\ALUC\ALUC Administrative Items\Admin. 2015\ADmin Item 04-09-15.doc



# AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY



March 2, 2015

**CHAIR**  
**Simon Housman**  
Rancho Mirage

**VICE CHAIRMAN**  
**Rod Ballance**  
Riverside

**COMMISSIONERS**

**Arthur Butler**  
Riverside

**John Lyon**  
Riverside

**Glen Holmes**  
Hemet

**Greg Pettis**  
Cathedral City

**Steve Manos**  
Lake Elsinore

**STAFF**

**Director**  
Ed Cooper

John Guerin  
Russell Brady  
Barbara Santos

County Administrative Center  
4080 Lerron St., 14<sup>th</sup> Floor.  
Riverside, CA 92501  
(951) 955-5132

[www.rcaluc.org](http://www.rcaluc.org)

Mr. Paul Swancott, Associate Planner  
City of Murrieta Planning Department  
One Town Square  
24601 Jefferson Avenue  
Murrieta, CA 92562

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW**

File No.: ZAP1060FV15  
Related File No.: DPO-2013-3335 (Development Plan)  
APN: 913-160-040

Dear Mr. Swancott:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC), staff reviewed the above- referenced proposal to develop 196 apartment units in nine buildings up to three stories in height, along with clubhouse and pool, on 11.07 gross acres (9.85 net acres) located southerly of Murrieta Hot Springs Road, easterly of Via Princessa, northerly of Calle Del Lago, and westerly of Agua Vista in the City of Murrieta.

The site is located in Airport Compatibility Zone D. Residential density in Airport Compatibility Zone D is restricted to either a maximum of 0.2 dwelling units per acre or a minimum of 5.0 dwelling units per acre. The project proposes a total of 196 units on the 9.85 net acre site for a density of 19.89 dwelling units per net acre, which is consistent with the higher range density criteria of a minimum of 5.0 dwelling units per acre.

Compatibility Zone D requires a minimum of 10% of open area. Based on the project area of 11.07 gross acres, a minimum of 1.107 acres of open area is required to be provided. The project's 79 foot half width and 588.85 foot frontage along Murrieta Hot Springs Road would qualify as open area. This half width area with an additional three foot area on-site outside of the Murrieta Hot Springs right-of-way would result in a total area of 1.108 acres. The project's perimeter fence/wall would be located three feet in from the right-of-way line and the open area would not include any trees or light poles which could be considered obstructions. Based on this, the proposed 1.108 acre open area would comply with the minimum 10% open area required in Compatibility Zone D.

The elevation of Runway 18-36 at its southerly terminus is approximately 1,330 feet above mean sea level (1330 feet AMSL). At a distance of approximately 7,240 feet from the runway to the property line, FAA review would be required for any structures with top of roof exceeding 1402.4 feet AMSL. The proposed maximum pad elevation is 1140 feet AMSL and the project includes proposed building elevations with a maximum height of 40 feet 4 inches for a total maximum elevation of 1180.3 feet AMSL. Therefore, Federal Aviation Administration (FAA) obstruction evaluation review would not be required.

As ALUC Director, I hereby find the above-referenced project CONSISTENT with the 2007 French Valley Airport Land Use Compatibility Plan as amended in 2011, subject to the following conditions:

**CONDITIONS:**

1. Any outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky.
2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area, including landscaping utilizing water features, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, and incinerators.
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached notice shall be provided to all potential purchasers of the property and all potential tenants of the apartments, and shall be recorded as a deed notice.
4. Any new retention basins on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the retention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
5. All open space areas as indicated on the exhibit titled Bel Air Apartments ZAP1060FV15 Area Exhibit shall be kept free of structures and other major obstacles such as walls, trees or poles (greater than 4 inches in diameter, measured 4 feet above the ground), and overhead wires. This shall apply to the initial development of the current project and in perpetuity if there is any consideration by the City to modify or add landscaping within the parkway or median for Murrieta Hot Springs Road.
6. Use of the on-site clubhouse and pool(s) shall be limited to residents and their guests.

March 2, 2015

If you have any questions, please contact Russell Brady, Contract Planner, at (951) 955-0549, or John Guerin, Principal Planner, at (951) 955-0982.

Sincerely,  
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Edward C. Cooper, Director

RB:bks

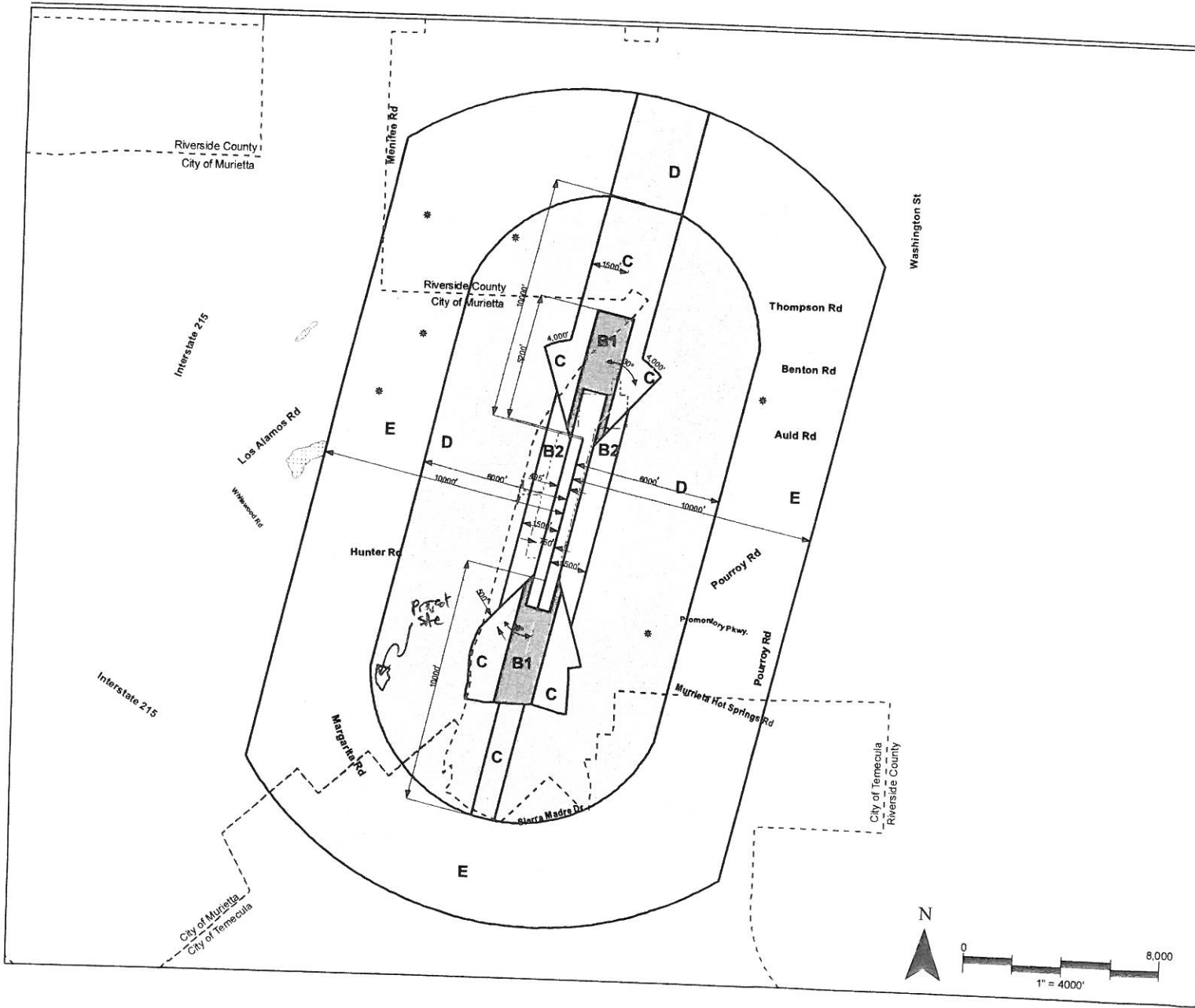
Attachment: Notice of Airport in Vicinity

cc: Bel Air Murrieta, LLC/Zomorrodian Services Co. (applicant)  
Tom Love (Engineer)  
Doug Heaton, KTG Y Group, Inc.  
Daryl Shippy, Riverside County Economic Development Agency – Aviation Division  
ALUC Case File

Y:\AIRPORT CASE FILES\French Valley\ZAP1060FV15\ZAP1060FV15.LTR.doc

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



**Legend**

- Compatibility Zones**
- Airport Influence Area Boundary
  - Zone A
  - Zone B1
  - Zone B2
  - Zone C
  - Zone D
  - Zone E
- Boundary Lines**
- - - Airport Property Line
  - - - City Limits
  - \* Height Review Overlay Zone

**Note**

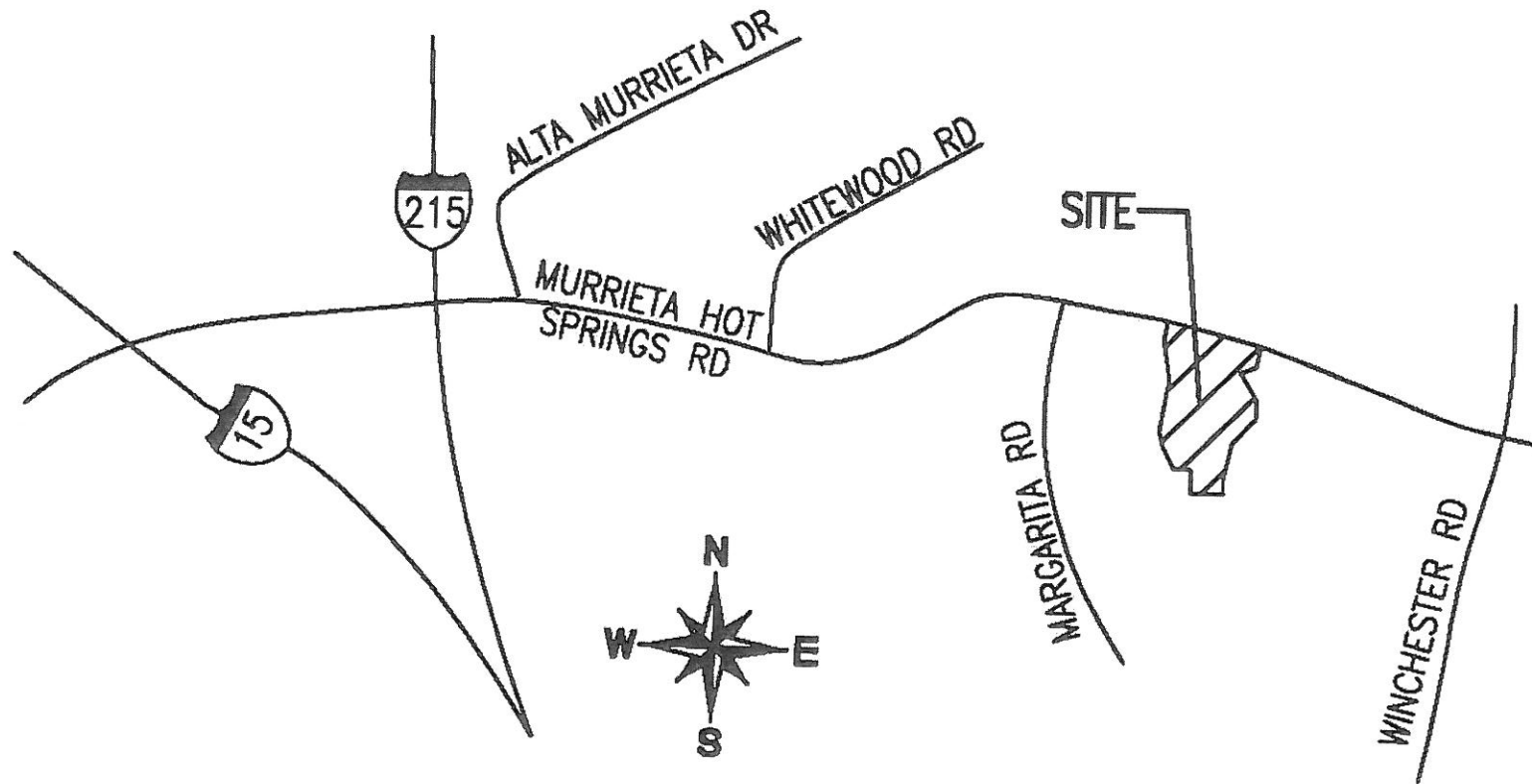
Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

See Chapter 2, Table 2A from compatibility criteria associated with this map.

Riverside County  
 Airport Land Use Commission  
 Riverside County  
 Airport Land Use Compatibility Plan  
 Policy Document  
 (April 2010)

Map FV-1

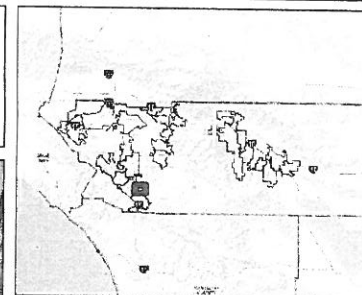
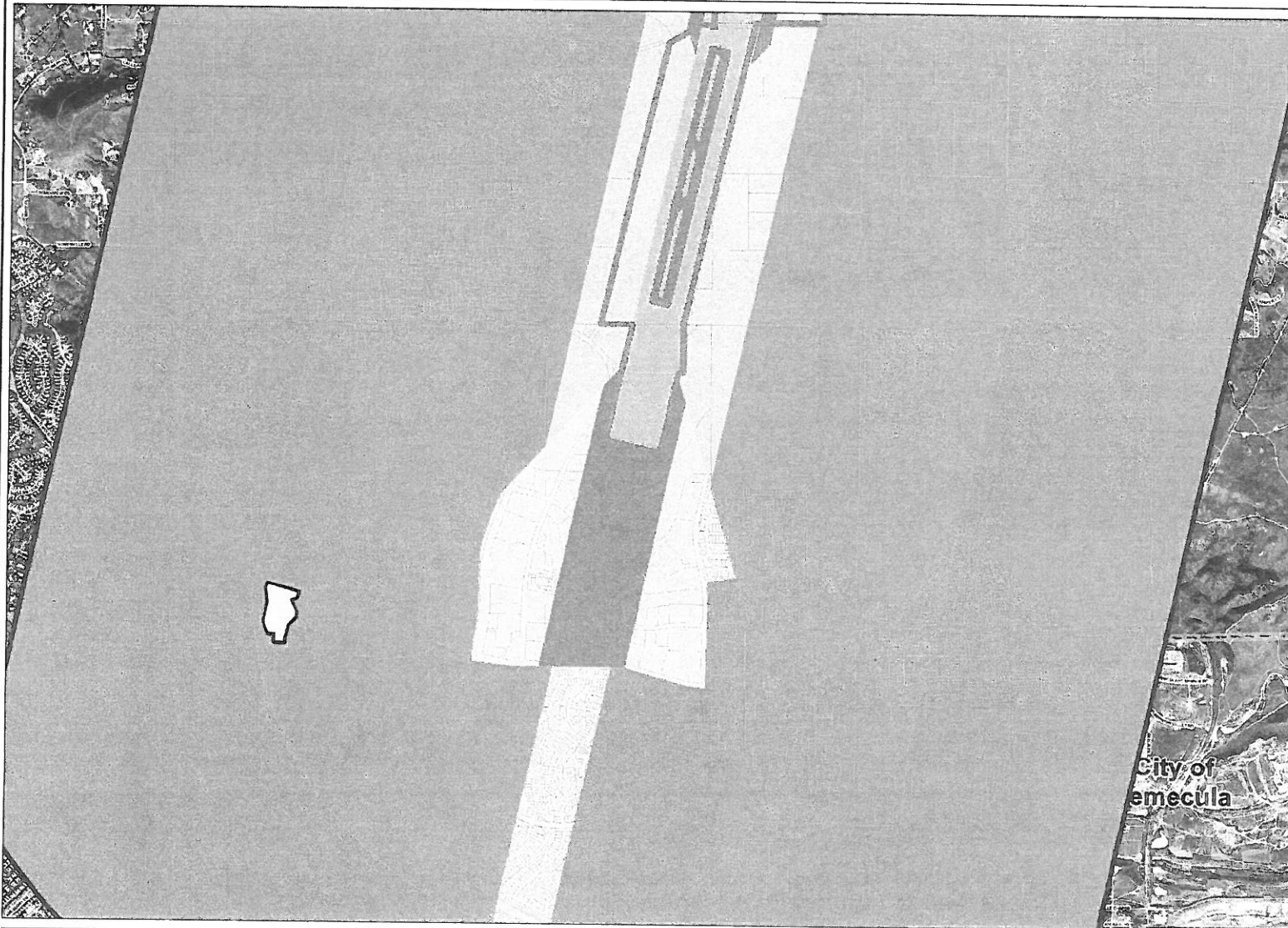
**Compatibility Map**  
 French Valley Airport



VICINITY MAP

NTS

# My Map



## Legend

- Airports
- AIA
- Airport Compatibility**
- <all other values>
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E
- Runways
- City Boundaries
- roadsanno**
- highways**
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads**
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities



0 3,280 6,559 Feet



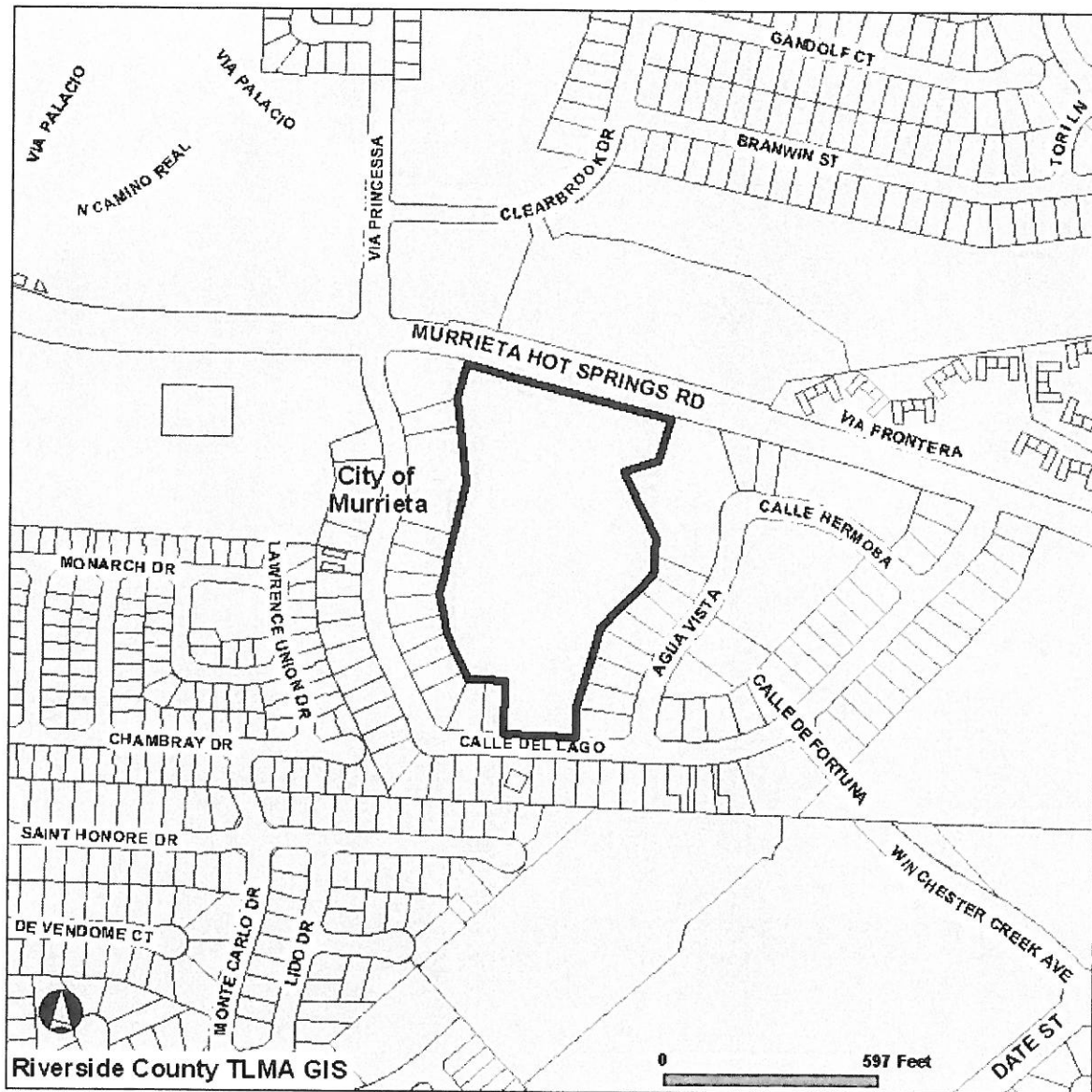
**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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## Notes

RIVERSIDE COUNTY GIS



Selected parcel(s):  
913-160-040

**AIRPORTS**

- SELECTED PARCEL
- AIRPORT INFLUENCE AREAS
- INTERSTATES
- HIGHWAYS
- PARCELS
- COMPATIBILTY ZONE D
- COMPATIBILTY ZONE E

**\*IMPORTANT\***

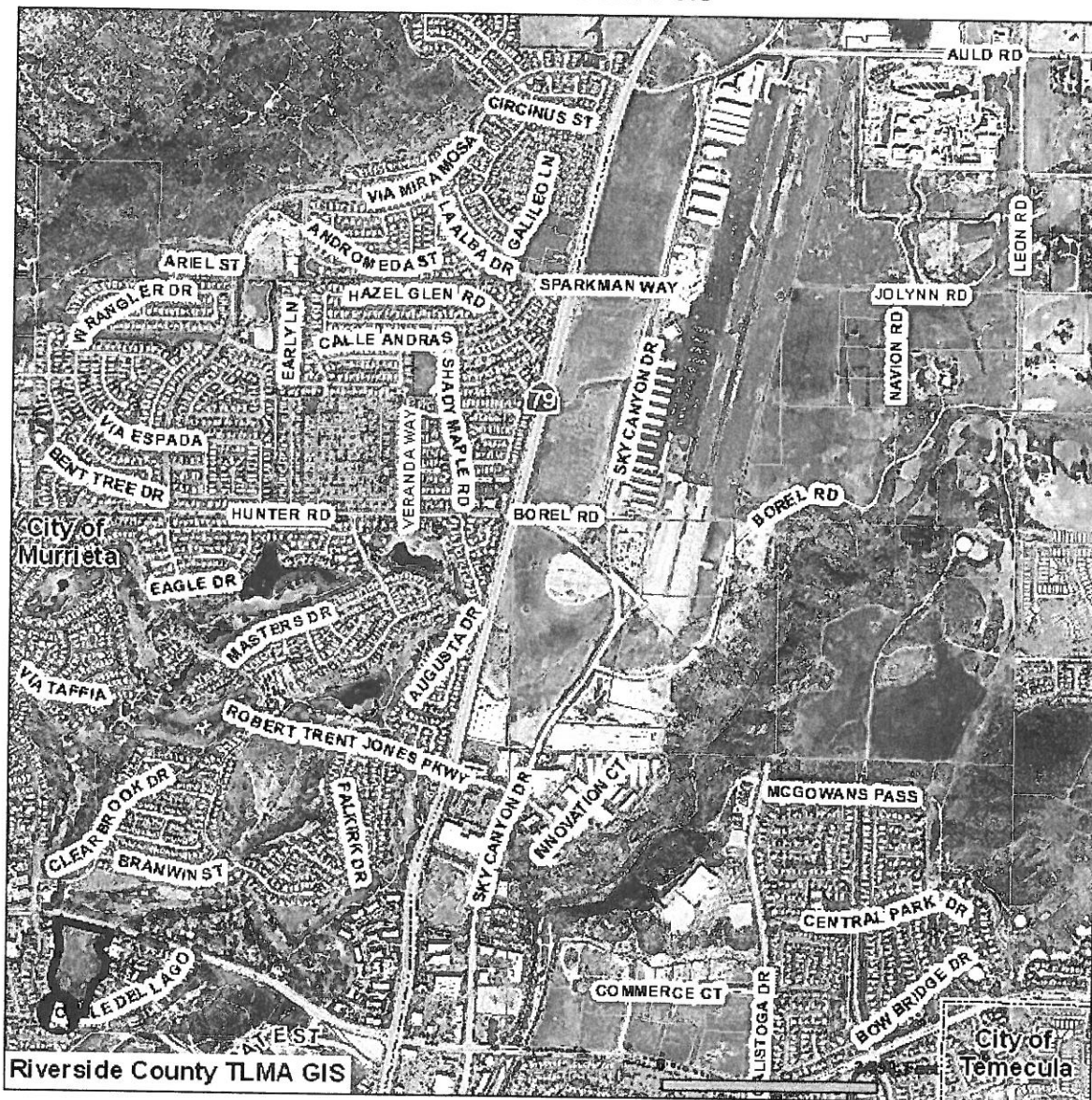
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Version 131127



RIVERSIDE COUNTY GIS



Selected parcel(s):  
913-160-040

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Version 131127

RIVERSIDE COUNTY GIS



Selected parcel(s):  
913-160-040

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Version 131127

RIVERSIDE COUNTY GIS



Selected parcel(s):  
913-160-040

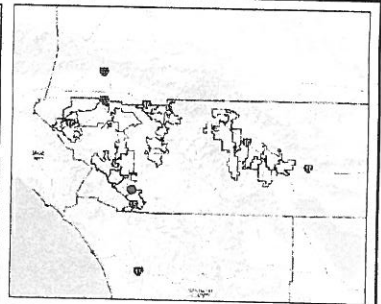
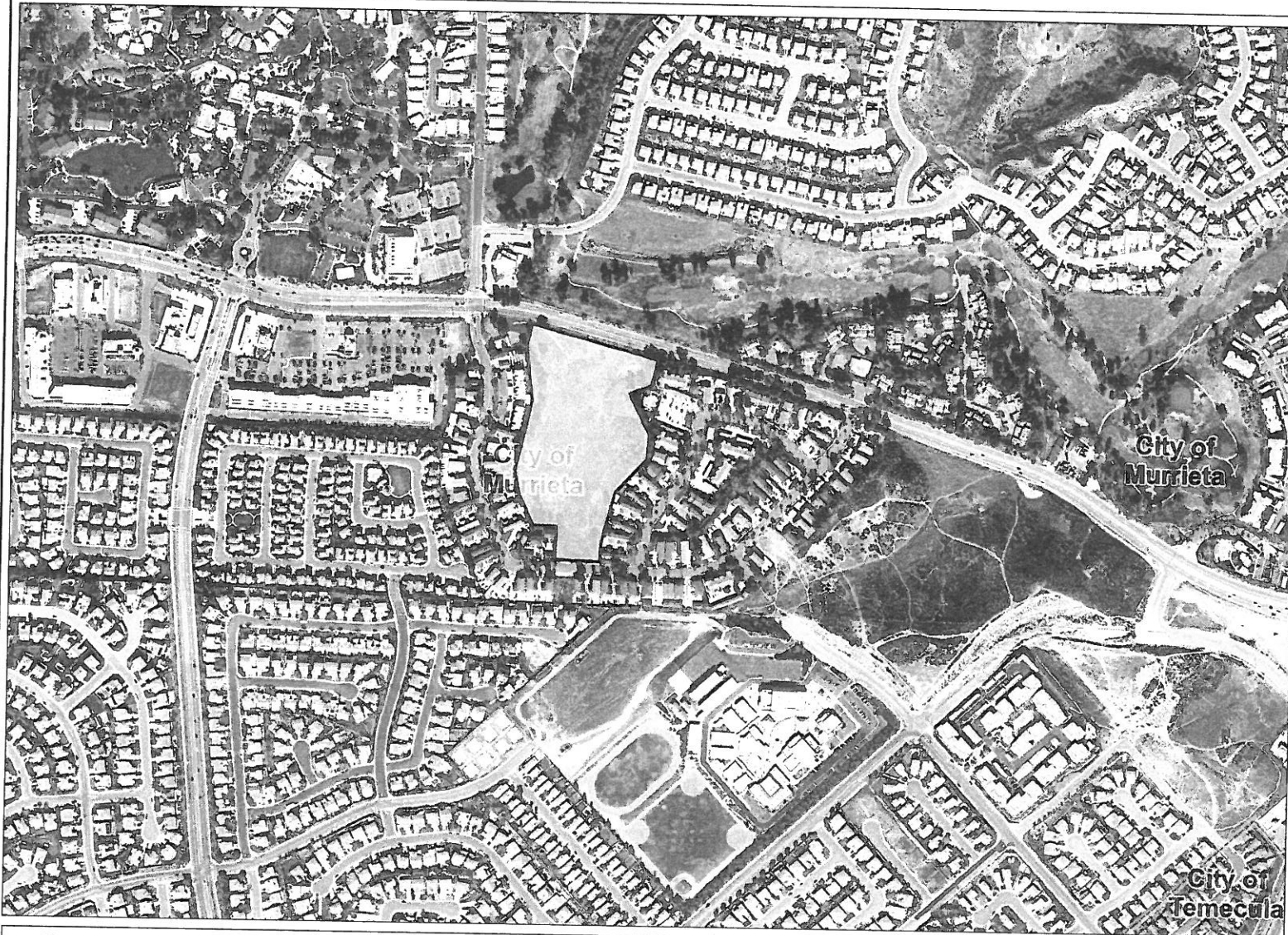
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Version 131127

# My Map



## Legend

-  RCLIS Parcels
-  City Boundaries
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
-  counties
-  cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 820 1,640 Feet

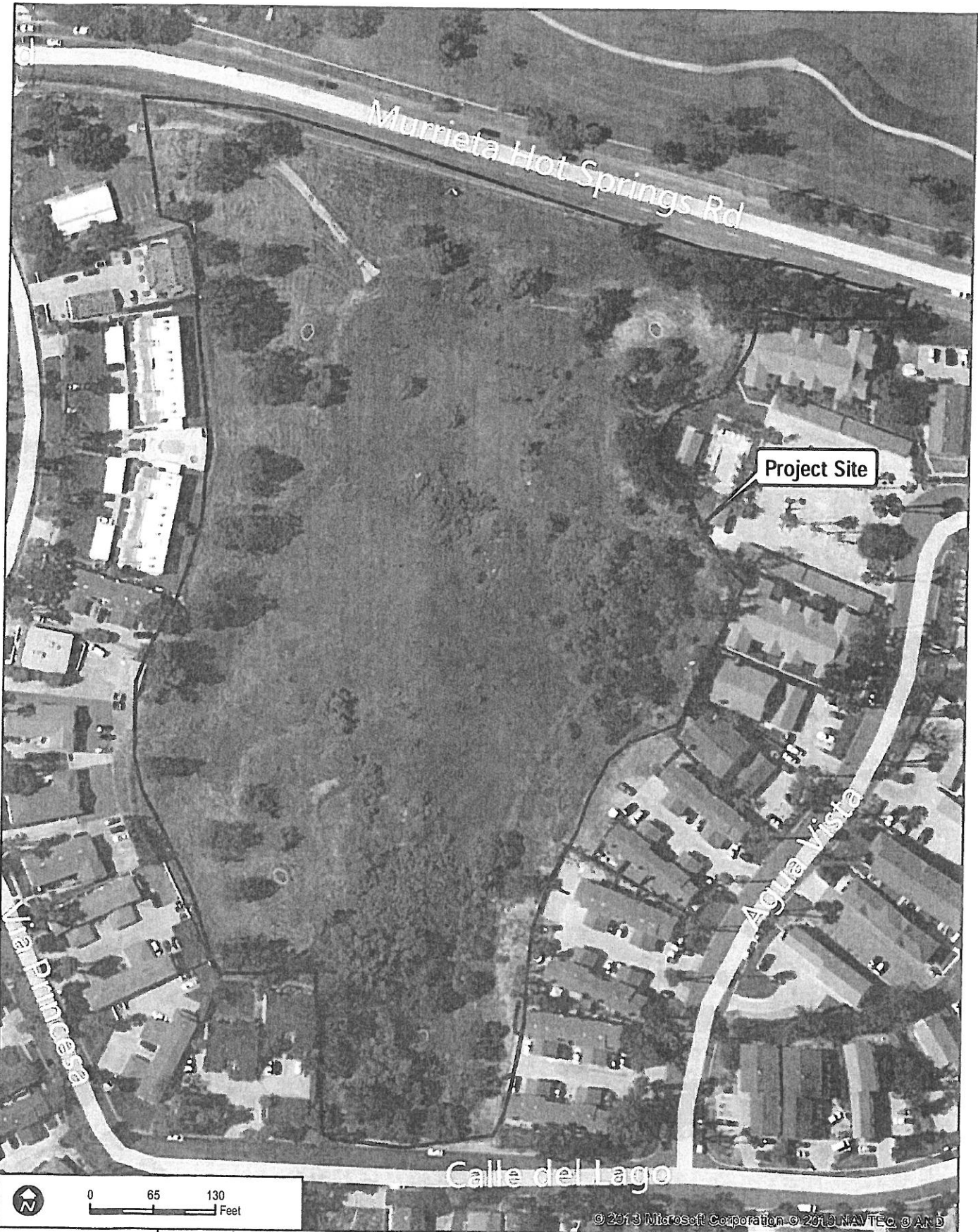


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## Notes



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**DUDEK**

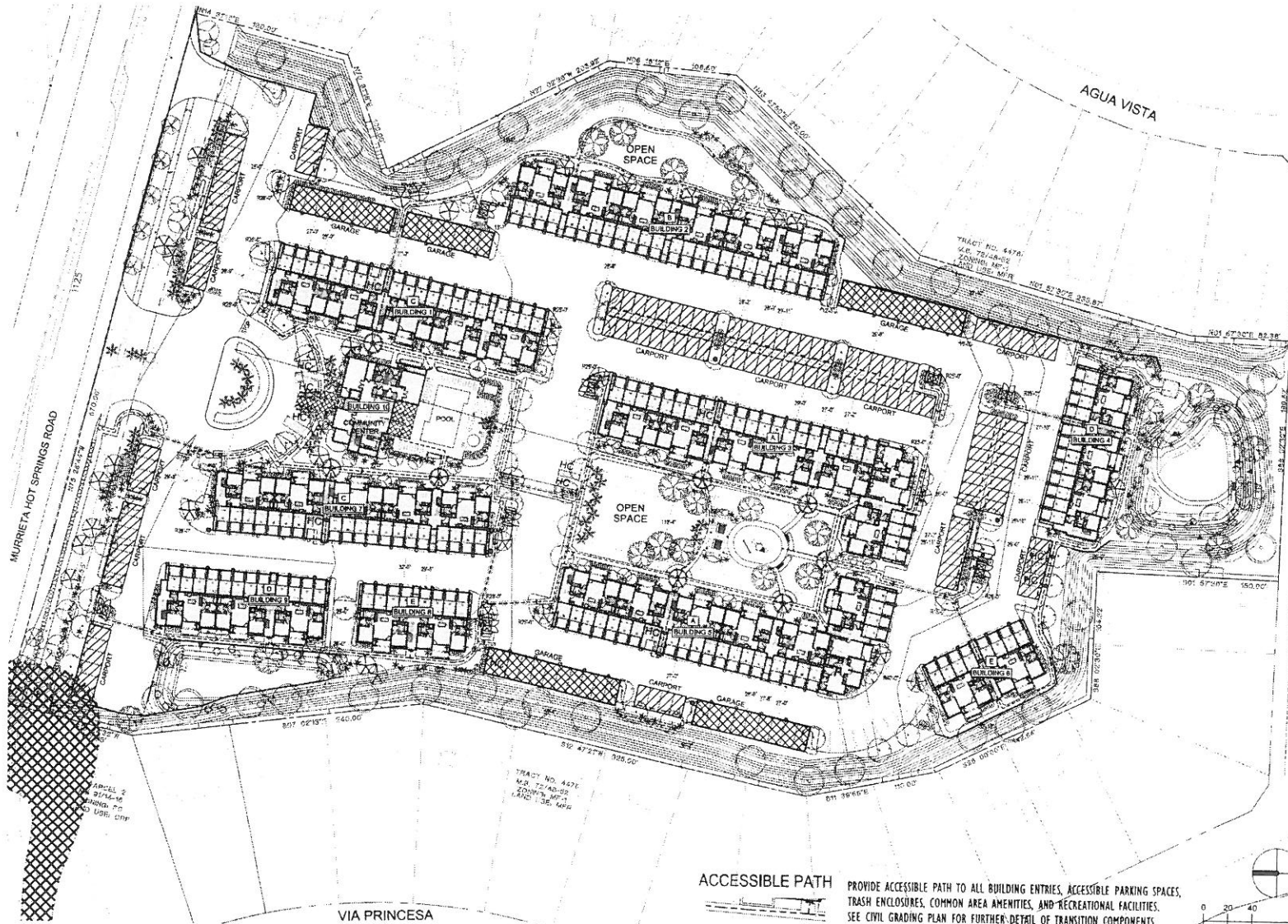
SOURCE: Bing Maps 2013

**FIGURE 2**  
**Aerial Map**

7396  
MAY 2013

10.91 ACRE PROPERTY - BEL AIR MURRIETTA LLC

Z:\Projects\7396\10\MAP\DOC\MAP\SP\Planning App\_Submittal\Fig 2 Aerial.mxd



**Project Summary**

**Project Description:**  
A 196 unit multi-family apartment project on a 9.85 acre parcel including nine apartment buildings, a clubhouse and pool, play area, and other bbg areas. The unit mix includes 1, 2, and 3 bedroom apartments ranging from 810 square feet to 1309 square feet.

**Legal Description:**  
All that portion of lots 188 and 190, Webster Ave. and Clinton Ave. and a portion of the Murrieta portion of the Temecula Rancho, as shown by the map of the Temecula land and water Company, on file in Book 8 Page 359 of maps, records of San Diego County, California

**Project Info:**  
Address: The site is located on Murrieta Hot Springs Road between Via Princessa and Calle Del Lago.  
APN: 913-50-040  
City Case #: DPC-2013-3335

**Site Area Information**

Net Site Area:	9.85 AC
Proposed Dwelling Units:	196 DU
Density Allowed:	15 - 18 DU/Net AC (177 DU)
Density Bonus = 20% increase for 10% lower income affordable housing inclusion per Murrieta MC 16.20.030 A.1, and B.2.	18 - 21 DU/Net AC (212 DU)
Proposed Density:	20 DU/Net AC (196 DU)
10% of Total Proposed DU to be Set-Aside for Lower Income	20 DU

**Open Space**

Required Common Open Space:	39,200 SF	(200SF / Unit)
Provided Common Open Space:	+ 45,000 SF	(230SF / Unit)
Required Private Open Space:	100SF/Gross Flr Unit, 805SF/Upper Flr Unit	
Provided Private Open Space:	100-1275SF/Gross Flr Units, 70-805SF/Upper Flr Units	
(see unit plan sheets A5.D.5.1)		

**Site Coverage Calculations**

Allowed Building Coverage:	150,935 SF (35%)
Proposed Building Coverage:	105,493 SF (24%)
Required Landscape Area:	42,829 SF (10%)
Provided Landscape Area:	154,180 SF (36.3%)
Total R.O.W. Area:	18,070 SF
Landscape in R.O.W.:	12,320 SF (68.2%)

**Unit Mix**

1 Bedroom:	76	(39%)
2 Bedroom:	114	(59%)
3 Bedroom:	6	(3%)
Total:	196 Units	

**Parking Required (w/ Density Bonus per Parking Option 1 of SB 1818):**

1 BR (7.6 x 1)	76
2 BR (11.4 x 2)	228
3 BR (6.2 x 2)	12
TOTAL REQUIRED	316 ONSITE SPACES

**Accessible Parking Required:**

Min. 2% of Assigned (316 x .02)	7 (1 Van)
Min. (1) of each type of assigned	
Min. 5% of (Unassigned) Visitor (0x.05)	0
TOTAL REQUIRED	7 (1 Van)

**Parking Provided:**

Garage (5 Accessible)	196
Carport (2 Accessible)	120
Surface (5 Accessible w/ 2 Van)	69
TOTAL PROVIDED	385 SPACES
	(12 Accessible w/ 2 Van)

**BEL AIR APARTMENT HOMES (MURRIETA 196)**

Rick Mashon & Kazem Zomorrodian

Rick P: (925) 354-9100  
Email: rick@diamondplanning.com  
Kazem P: (760) 745-7000  
Email: infozomorrodian@rthob.com

**ARCHITECTURAL SITE PLAN**

Project No. 196-0001  
Date: 08/21/2013  
Scale: 1/8" = 1'-0"

KTGY Group, Inc.  
Architecture+Planning  
1733 Green Ave., Suite 200  
Santa Monica, CA 90401  
310.394.2023  
ktgy.com

**A1.5**



**PROJECT SUMMARY**

**SITE INFORMATION:**  
 SITE AREA: 10.91 GROSS AC.  
 DENSITY: 17.965 DU/AC  
 ZONE: MF-2

\*NOTE: PROPOSED PROJECT  
 NOT LOCATED IN FAULT ZONE

**BUILDING SUMMARY:**

BUILDING	1 BR	2 BR	3 BR	GARAGE SPACES	TOTAL BUILDINGS
A	12	18	3	24	2
B	12	18	-	24	1
C	10	15	-	20	2
D	6	9	-	12	2
E	4	6	-	8	2
CLUBHOUSE					+/-5.3205F

**UNIT MIX:**

1 BEDROOM	76	(39%)
2 BEDROOM	114	(58%)
3 BEDROOM	6	(3%)
<b>TOTAL</b>	<b>196</b>	

**PARKING REQUIRED:**

1 BR (76 x 1.5)	114
2 BR (114 x 2)	228
3 BR (6 x 2.5)	15
GUEST (196 x .25)	49
<b>TOTAL REQUIRED</b>	<b>406 SPACES</b> (210 SURFACE SPACES) (196 GARAGE SPACES)

**PARKING PROVIDED:**

GARAGE	196
SURFACE	210
<b>TOTAL</b>	<b>406 SPACES</b>

CITY OF MURRIETA  
 Community Development/Planning Dept.  
 RECEIVED

JUN 17 2013

CASE #  
DP-2013-3335



**MURRIETA 196**

**Rick Mashon & Kazem Zomorrodian**

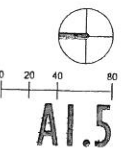
Rick P: (323) 584-9500  
 Email: rick@bonappetitbakery.com  
 Kazem P: (760) 845-3093  
 Email: mkzomorrodian@yahoo.com

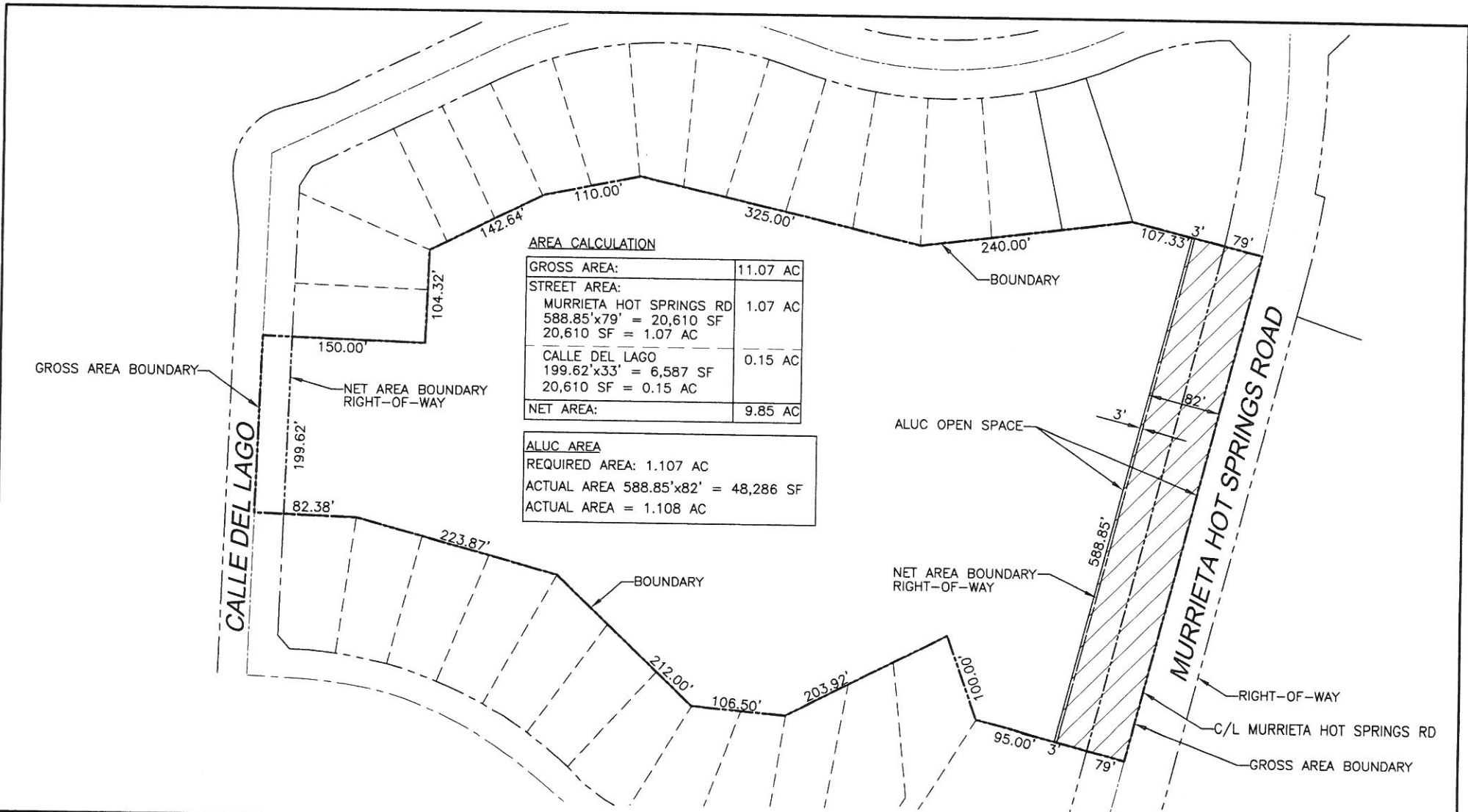
**ARCHITECTURAL SITE PLAN**

Murrieta, CA  
 EDD # 2011-0134

05.31.2013

**KTGY Group, Inc.**  
**Architecture+Planning**  
 17922 Fitch  
 Irvine, CA 92614  
 949.851.2133  
 ktgy.com





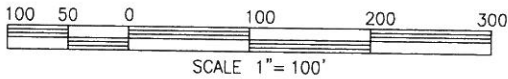
**AREA CALCULATION**

GROSS AREA:	11.07 AC
STREET AREA:	
MURRIETA HOT SPRINGS RD 588.85'x79' = 20,610 SF 20,610 SF = 1.07 AC	1.07 AC
CALLE DEL LAGO 199.62'x33' = 6,587 SF 20,610 SF = 0.15 AC	0.15 AC
NET AREA:	9.85 AC

**ALUC AREA**

REQUIRED AREA: 1.107 AC
ACTUAL AREA 588.85'x82' = 48,286 SF
ACTUAL AREA = 1.108 AC

**LOVE ENGINEERING**  
 PLANNING ♦ ENGINEERING ♦ SURVEYING  
 31915 RANCHO CALIFORNIA ROAD/SUITE 200-166  
 TEMECULA, CA 92591  
 TEL (951) 440 - 8149 / FAX (951) 303 - 6701



**LEGEND**  
 ALUC OPEN SPACE AREA 

**BEL AIR APARTMENTS**  
 ZAP1060FV14  
 AREA EXHIBIT



**NOTES:**

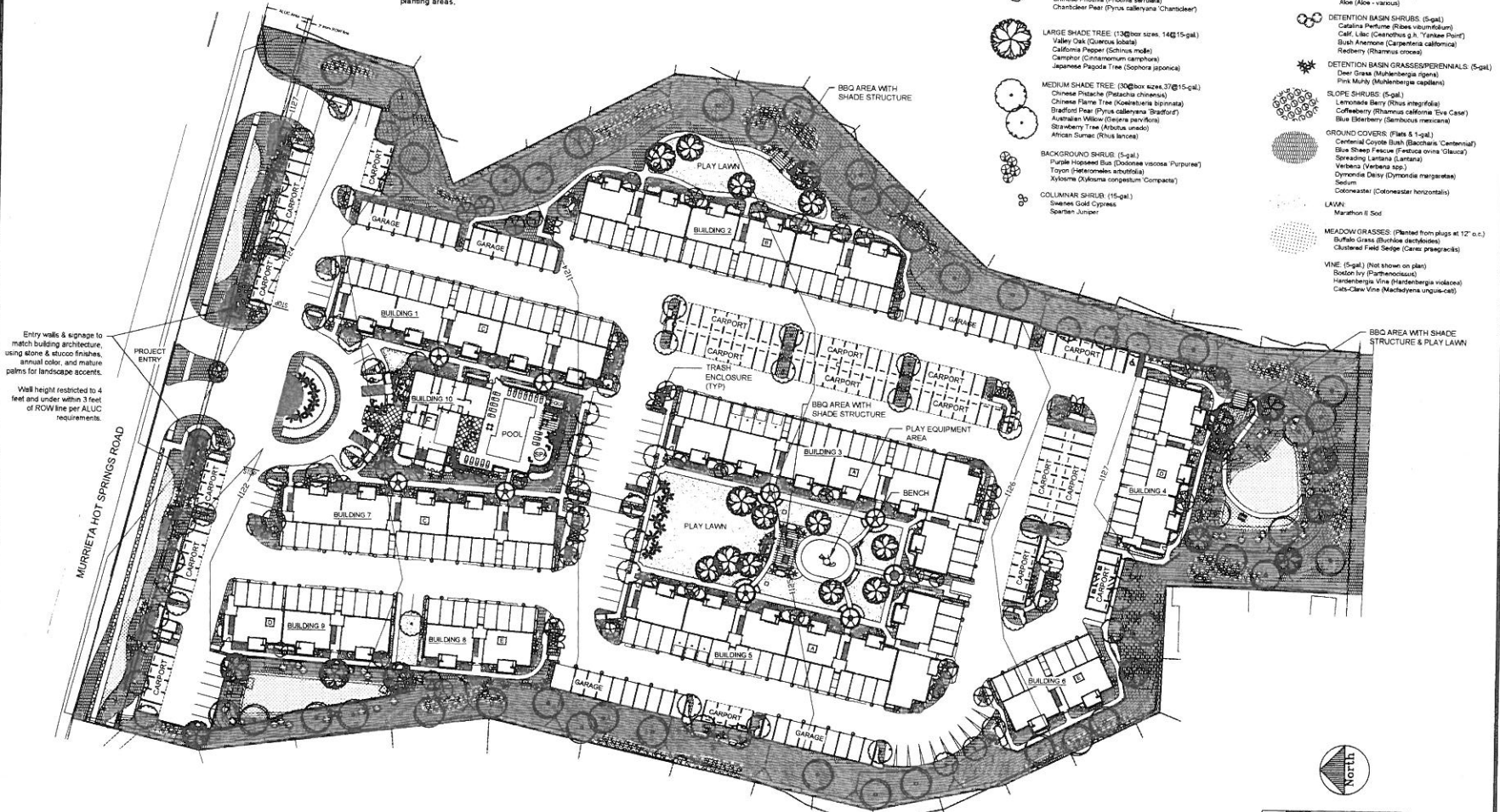
- All final landscape plans shall comply with all applicable codes of the Murrieta Municipal Code - Title 16.28.
- Per Airport Land Use Commission requirements, plants from the curb line to 3 feet behind the R.O.W. line shall have a maximum height of 4 feet.
- All existing trees on site will be removed.
- Tree sizes shall total minimum 35% box sizes, 65% 15-gal.
- A minimum 12" paved area shall be installed adjacent to end parking stalls.
- Irrigation system shall meet City standards and incorporate drip irrigation where required.
- Slope and basin plantings are to be low water use hydrozones per WUCOLS ratings. All other areas to be medium or low water use hydrozones.
- All transformers shall be screened with minimum 5 feet planting areas.

**AREA TABULATIONS**

428,290 s.f. Total Private Property	164,180 s.f. (38.3%) landscaped
18,070 s.f. Total ROW Area	12,320 s.f. (68.2%) landscaped
446,360 s.f. TOTAL AREA (10.37 acres)	176,500 s.f. (39.5%) landscaped

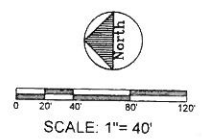
**PLANT LIST/LEGEND**

- PALMS:** (43 @ BT sizes as listed)
  - Queen Palm (*Syagria romanzoffiana*) min. 10-ft.
  - Date Palm (*Phoenix* spp.) min. 12-ft.
- SLOPE TREE:** (41 @ 15-gal)
  - Coast Live Oak (*Quercus agrifolia*)
  - California Pepper (*Schinus molle*)
  - Desert Willow (*Chilopsis linearis*)
  - Valley Oak (*Quercus lobata*)
  - Teju Tree (*Touaia* spp.)
- ACCENT TREE:** (37 @ box sizes)
  - Desert Willow (*Chilopsis linearis*)
  - Indian Hawthorn (*Rhodaphysa indica*) Majestic Beauty
  - Crape Myrtle (*Lagerströmia indica*)
  - Redbud (*Cercis occidentalis*)
  - Citrus Trees (*Citrus* spp.)
- NARROW UPRIGHT TREE:** (20 @ box sizes 10 @ 15-gal)
  - Fruitless Sweet Gum (*Liquidambar styraciflua*)
  - Chinese Photinia (*Photinia serrulata*)
  - Chantrelle Palm (*Pyrus calleryana*) Chantrelle
- LARGE SHADE TREE:** (13 @ box sizes 14 @ 15-gal)
  - Valley Oak (*Quercus lobata*)
  - California Pepper (*Schinus molle*)
  - Gambel (*Cinnamomum camphora*)
  - Japanese Pagoda Tree (*Sopora japonica*)
- MEDIUM SHADE TREE:** (30 @ box sizes 37 @ 15-gal)
  - Chinese Parasol (*Pteris chinensis*)
  - Chinese Flame Tree (*Koelbanera bipinnata*)
  - Bradford Pear (*Pyrus calleryana*) Bradford
  - Australian Willow (*Salix pyramidalis*)
  - Strawberry Tree (*Arbutus undata*)
  - African Sumac (*Rhus lancea*)
- BACKGROUND SHRUB:** (5-gal)
  - Purple Hopwood Bush (*Dodonaea viscosa*) Purple
  - Toyon (*Heteromeles arbutifolia*)
  - Xylorhiza (*Xylorhiza congesta*) Compact
- COLUMNAR SHRUB:** (15-gal)
  - Spartan Juniper
- MEDIUM SHRUB:** (5-gal)
  - Limelight (*Limnoria* spp.)
  - Coast Rosemary (*Hebe* spp.)
  - Compact Myrtle (*Myrica communis*) Compact
  - Landscape Roses (*Rosa* hybrids)
  - Rockrose (*Cistus* spp.)
  - Green and Gold Drift (*Eucropea virens*)
  - Texas Ranger (*Leucophyllum frutescens*)
  - Ornamental grasses (various)
- LOW SHRUBS & PERENNIALS:** (1-gal & 5-gal)
  - Daylily (*Hemerocallis* hybrids)
  - Society Garlic (*Tulbaghia violacea*)
  - Autumn Sage (*Salvia greggii*)
  - Pacific Noddy Manzanita (*Arctostaphylos Pacifica* var.)
  - Howard Manzanita (*Arctostaphylos Howard* McMin)
  - Yankee Point Calif. Lilac (*Ceanothus Yankee Point*)
  - Agave (*Agave attenuata*)
  - Aloe (*Aloe* -various)
- DETENTION BASIN SHRUBS:** (5-gal)
  - California Perfume (*Ribes viscidifolium*)
  - Calif. Lilac (*Ceanothus* s.p.) Yankee Point
  - Bush Anemone (*Carpenteria californica*)
  - Redberry (*Rhamnus crocea*)
- DETENTION BASIN GRASSES/PERENNIALS:** (5-gal)
  - Pink Blunt (*Muhlenbergia capillaris*)
  - Deer Grass (*Muhlenbergia rigens*)
  - Blue Silphium (*Sanicula maritima*)
- SLOPE SHRUBS:** (5-gal)
  - Coffeeberry (*Rhamnus californica*) Eve Case
  - Blue Silphium (*Sanicula maritima*)
- GROUND COVERS:** (Plants & 1-gal)
  - Central Coyote Bush (*Baccharis Californica*)
  - Blue Sheep Fescue (*Festuca ovina*) Glaucous
  - Spreading Lentenita (*Lentenita*)
  - Verbena (*Verbena* spp.)
  - Dormant Daisy (*Dymalis margaritacea*)
  - Sedum
  - Cottoncandy (*Colonnella horizontalis*)
- LAVAS:**
  - Marathon II Sod
- MEADOW GRASSES:** (Planted from plugs at 12" o.c.)
  - Buffalo Grass (*Bouteloua dactyloides*)
  - Clustered Field Sedge (*Carex praeprae*)
- VINE:** (5-gal) (Not shown on plan)
  - Boston Ivy (*Parthenocissus*)
  - Hardenbergia Vine (*Hardenbergia violacea*)
  - Cats-Claw Vine (*Macleodena unguis-cati*)



Entry walls & signage to match building architecture, using stone & stucco finishes, annual color, and mature palms for landscape accents.

Wall height restricted to 4 feet and under within 3 feet of ROW line per ALUC requirements.



REVISIONS	BY

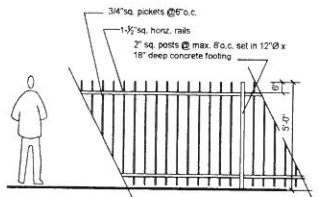
**ROYAL OAK DESIGN**  
 Sybil E. Lyons, Landscape Architect #1549  
 2456 Hummingbird Way  
 La Verne, CA 91750-2371  
 Telephone: (909) 953-1158

**CONCEPTUAL LANDSCAPE PLAN**  
 198 APARTMENT UNITS  
 1100 HOT SPRINGS ROAD, MURRIETA, CA  
 APR. 9/13-16/20  
 REG. 448 MURRIETA, CA  
 4525 DISTRICT BLVD., VERNON, CA 92088

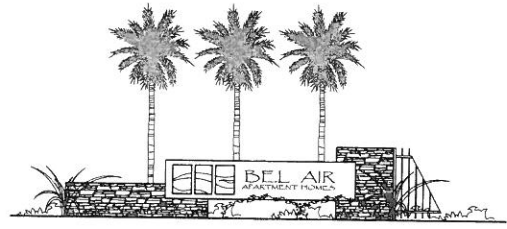


SCALE	1"= 40'
SHEET	2-24-2015
DATE	11/20/15
PROJECT	198 APARTMENT UNITS
LOCATION	1100 HOT SPRINGS ROAD, MURRIETA, CA
DATE	APR. 9/13-16/20
SCALE	1"= 40'
PROJECT	198 APARTMENT UNITS
LOCATION	1100 HOT SPRINGS ROAD, MURRIETA, CA
DATE	APR. 9/13-16/20
SCALE	1"= 40'
SHEET	2-24-2015

L-1



TUBULAR STEEL FENCE DETAIL  
SCALE: 3/8"=1'-0"

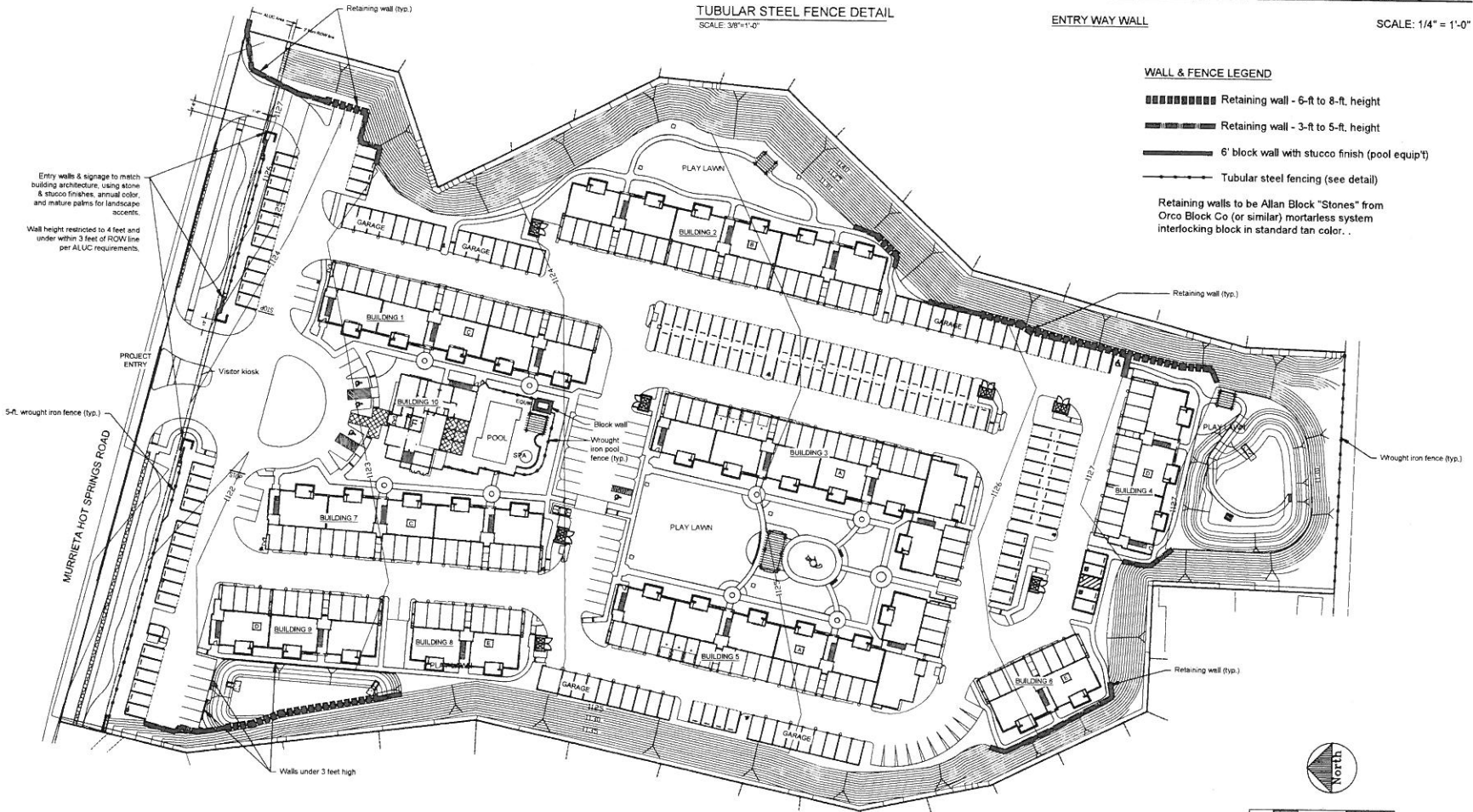


ENTRY WAY WALL  
SCALE: 1/4" = 1'-0"

WALL & FENCE LEGEND

- █ Retaining wall - 6-ft to 8-ft. height
- █ Retaining wall - 3-ft to 5-ft. height
- █ 6' block wall with stucco finish (pool equip't)
- Tubular steel fencing (see detail)

Retaining walls to be Allan Block "Stones" from Orco Block Co (or similar) mortartless system interlocking block in standard tan color.



Entry walls & signage to match building architecture, using stone & stucco finishes, annual color, and mature palms for landscape accents.  
Wall height restricted to 4 feet and under within 3 feet of ROW line per AUC requirements.



SCALE: 1"= 40'

REVISIONS	BY

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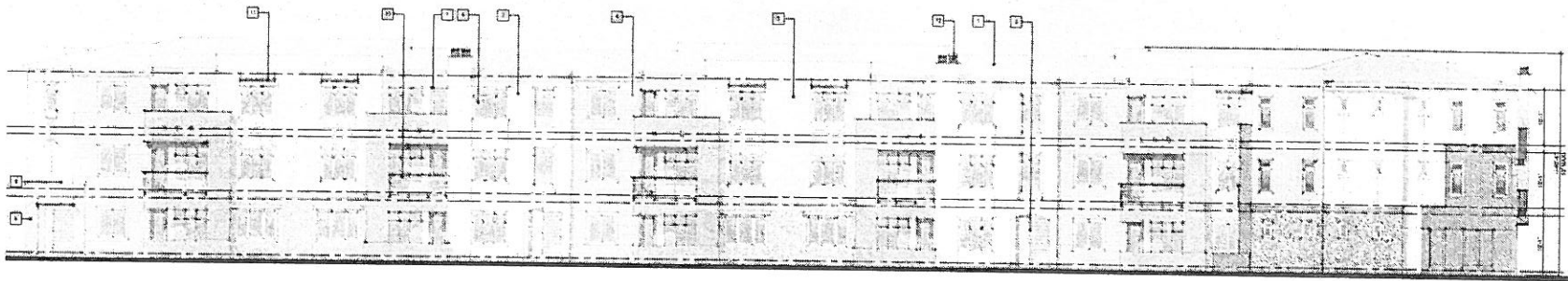
**ROYAL OAK DESIGN**  
 Sybil E. Lyons, Landscape Architect #1549  
 2456 Hummingbird Way  
 La Verne, CA 91750-2371  
 Telephone: (909)593-4158

**CONCEPTUAL WALL/FENCE PLAN**  
**196 APARTMENT UNITS**  
 38700 MURRIETA HOT SPRINGS ROAD, MURRIETA, CA  
 APR. 9/13-16-04  
 BEL AIR MURRIETA, LLC  
 4523 SOUTHERN BLVD., VERNON, CA 90055

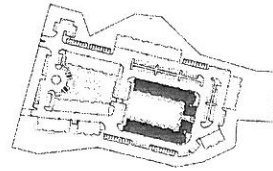


DATE: 2-24-2015  
 SHEET NO: 196-01  
 SCALE: 1"= 40'  
 DRAWN BY: J. MURRIETA  
 CHECKED BY: J. MURRIETA

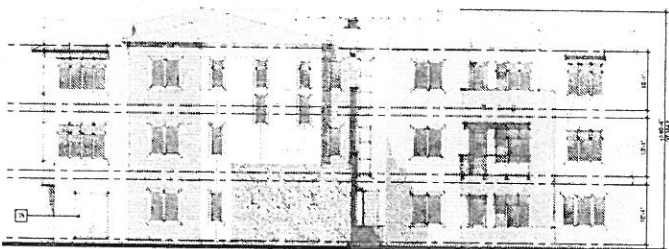
L-2



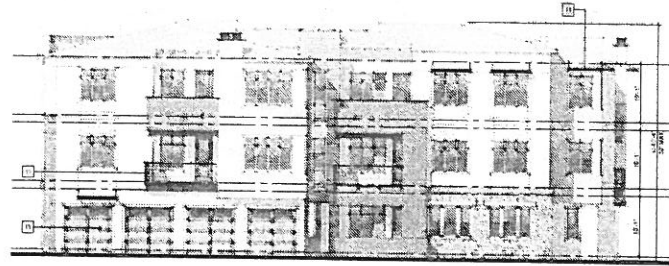
Front Elevation



KEY PLAN (N.T.S.)



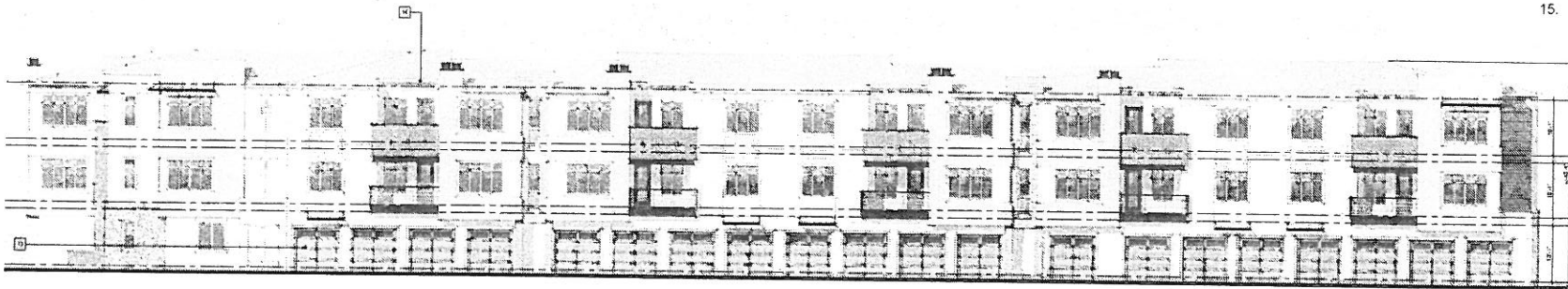
Left Side Elevation



Right Side Elevation

**Material Legend**

1. FLAT CONCRETE TILE ROOFING
2. 20/30 SAND FINISH STUCCO
3. FIBER-CEMENT LAP SIDING
4. FIBER-CEMENT BOARD & BATTEN
5. DECORATIVE STONE VENEER
6. RECESSED WINDOW
7. PRIVATE UNIT PATIO FRENCH DOOR
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12. DECORATIVE METAL CHIMNEY CAP
13. METAL SECTIONAL GARAGE DOOR
14. WOOD FASCIA BOARD
15. (RAFTER TAILS WHERE SHOWN) METER/UTILITY CABINET DOORS



Rear Elevation

**BEL AIR APARTMENT HOMES (MURRIETA 196)**

**Rick Mashon & Kazem Zomorrodian**

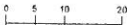
Rick P: (323) 584-9500  
 Email: rick@bonappetitbakery.com  
 Kazem P: (760) 845-3093  
 Email: mkzomorrodian@yahoo.com

**BUILDING A ELEVATIONS**

Murrieta, CA  
 CITY # 2011-0124

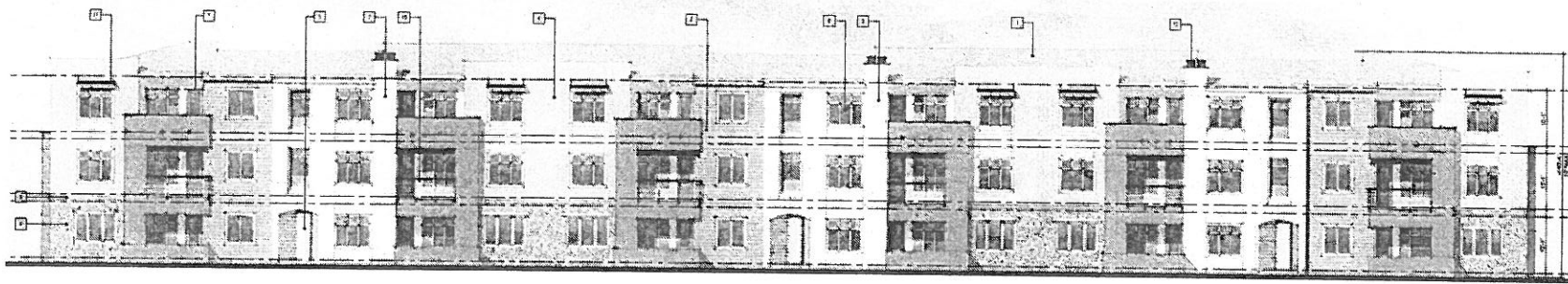
05.08.2014 3rd SUBMITTAL  
 01.30.2014 2nd SUBMITTAL  
 05.31.2013 1st SUBMITTAL

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 Architecture+Planning  
 1733 Ocean Ave., Suite 250  
 Santa Monica, CA 90401  
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 ktgy.com

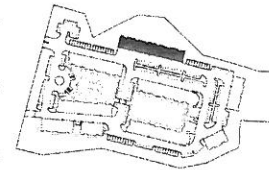


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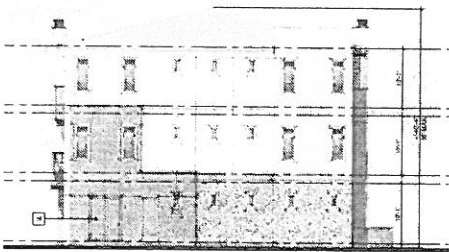




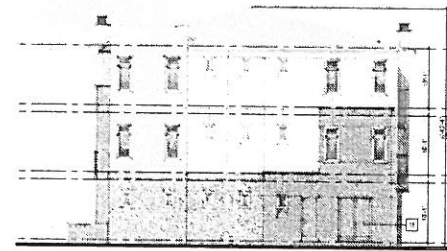
Front Elevation



KEY PLAN (N.T.S.)



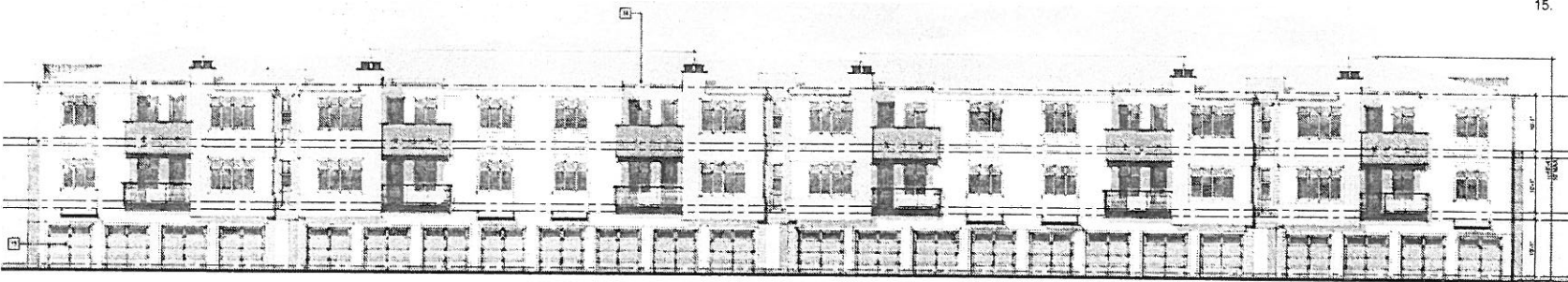
Left Side Elevation



Right Side Elevation

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Rear Elevation

**BEL AIR APARTMENT HOMES (MURRIETA 196)**

**Rick Mashon & Kazem Zomorrodian**

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 Email: rick@bonappetitbakery.com  
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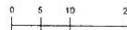
**BUILDING B ELEVATIONS**

Murrieta, CA

EGD #: 2011-0124

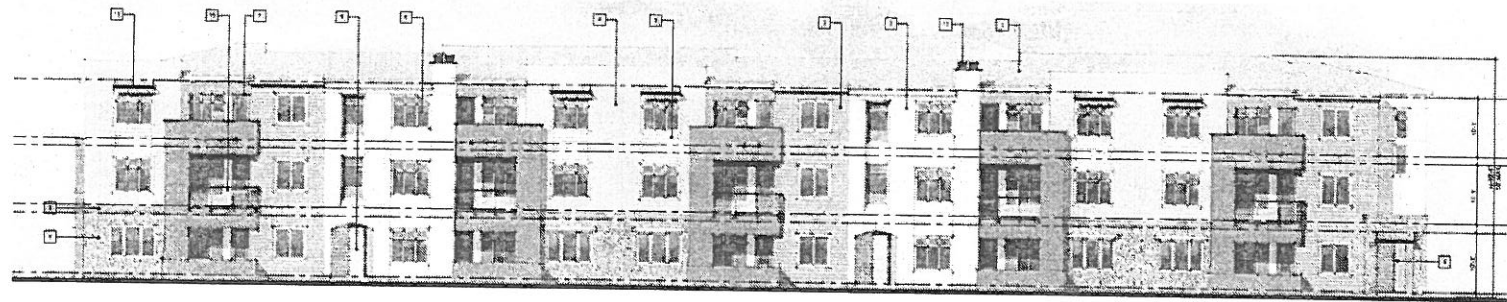
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 01.30.2014 2/4 SUBMITTAL  
 05.31.2013 1/4 SUBMITTAL

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 ktgy.com

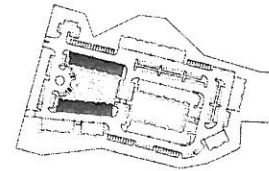


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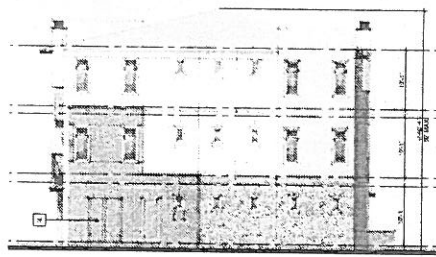




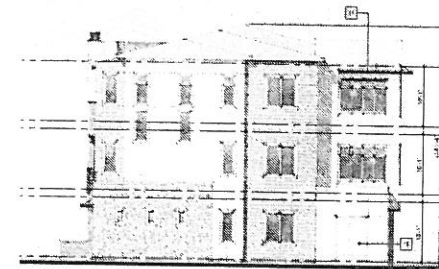
Front Elevation



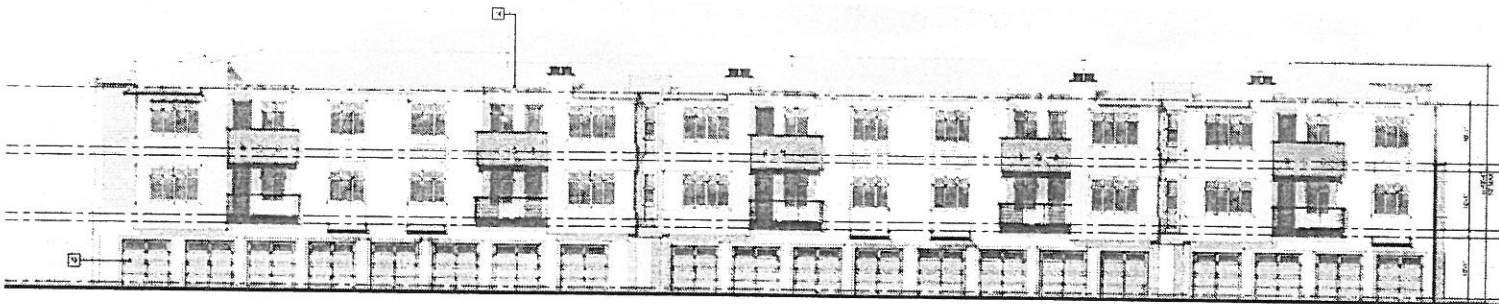
KEY PLAN (N.T.S.)



Left Side Elevation



Right Side Elevation



Rear Elevation

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15. METER/UTILITY CABINET DOORS

**BEL AIR APARTMENT HOMES (MURRIETA 196)**

**Rick Mashon & Kazem Zomorrodian**

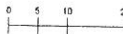
Rick P: (323) 584-9500  
 Email: rick@bonappetitbakery.com  
 Kazem P: (760) 845-3093  
 Email: mkzomorrodian@yahoo.com

**BUILDING C ELEVATIONS**

Murrieta, CA

KTGY # 2011-0124

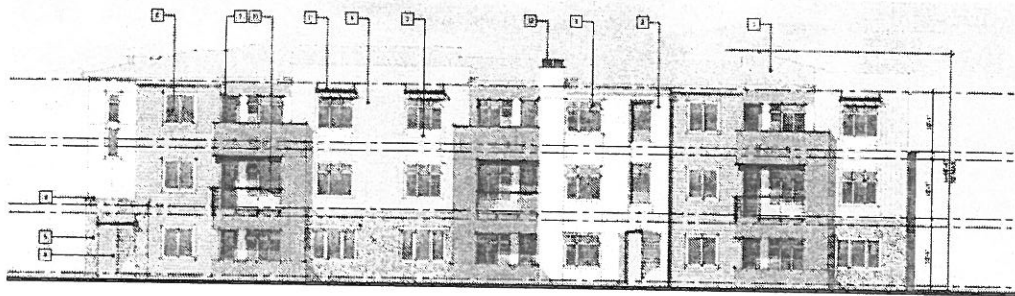
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 01.10.2014 2nd SUBMITTAL  
 05.31.2013 1st SUBMITTAL



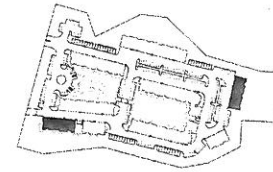
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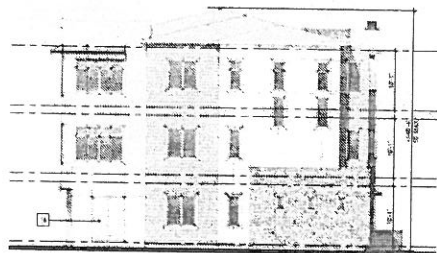
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 Architecture+Planning  
 1733 Ocean Ave., Suite 250  
 Santa Monica, CA 90401  
 310.394.2623  
 ktgy.com



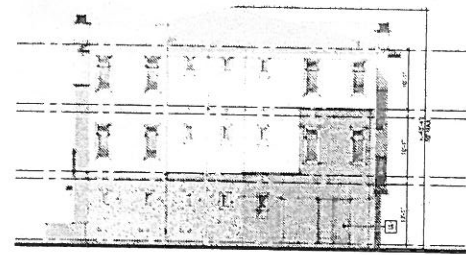
Front Elevation



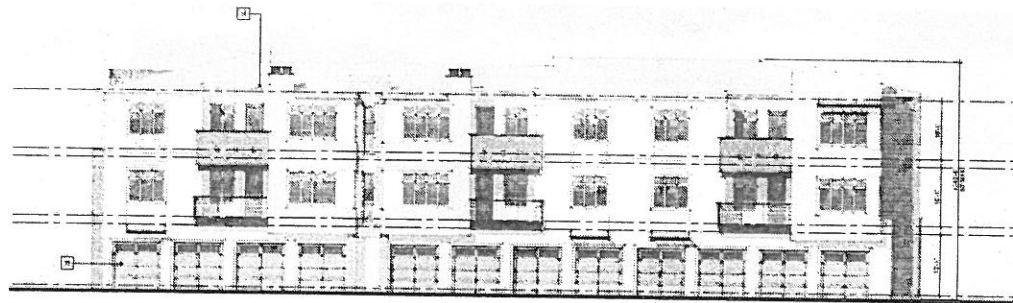
KEY PLAN (N.T.S.)



Left Side Elevation



Right Side Elevation



Rear Elevation

**Material Legend**

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**BEL AIR APARTMENT HOMES (MURRIETA 196)**

**Rick Mashon & Kazem Zomorrodian**

Rick P: (323) 584-9500

Email: rick@bonappetitbakery.com

Kazem P: (760) 845-3093

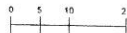
Email: mkzomorrodian@yahoo.com

**BUILDING D ELEVATIONS**

Murrieta, CA

KTY # 2011-0124

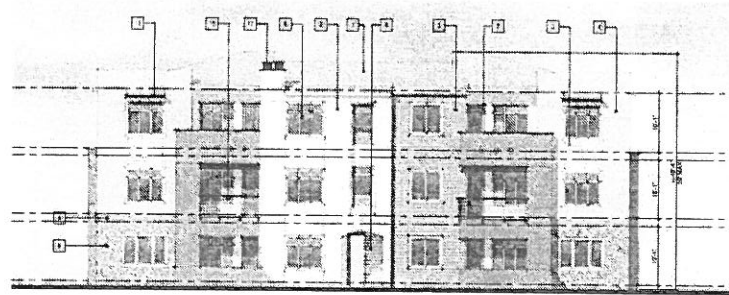
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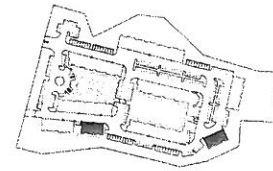
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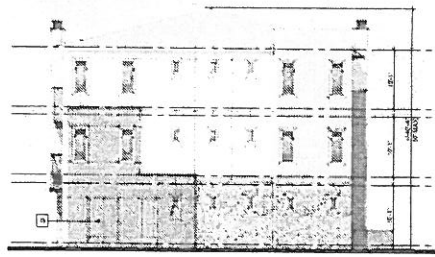
**KTGY Group, Inc.**  
**Architecture+Planning**  
 1733 Ocean Ave., Suite 250  
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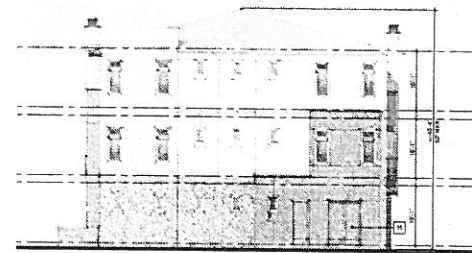
Front Elevation



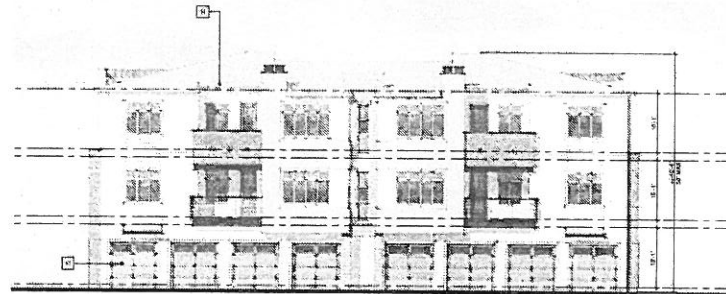
KEY PLAN (N.T.S.)



Left Side Elevation



Right Side Elevation



Rear Elevation

**Material Legend**

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**BEL AIR APARTMENT HOMES (MURRIETA 196)**

**Rick Mashon & Kazem Zomorrodian**

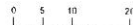
Rick P: (323) 584-9500  
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 Kazem P: (760) 845-3093  
 Email: mkzomorrodian@yahoo.com

**BUILDING E ELEVATIONS**

Murrieta, CA

KTG # 2011-0124

05.08.2014 3rd SUBMITTAL  
 01.30.2014 2nd SUBMITTAL  
 05.31.2013 1st SUBMITTAL



**A3.9**



KTGY Group, Inc.  
 Architecture+Planning  
 1733 Ocean Ave., Suite 250  
 Santa Monica, CA 90401  
 310.394.2623  
 ktgy.com

PAGE BREAK







# AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

**CHAIR**  
**Simon Housman**  
Rancho Mirage

March 2, 2015

**VICE CHAIRMAN**  
**Rod Ballance**  
Riverside

Ms. Julia Descoteaux, Associate Planner  
City of Moreno Valley Planning Department  
14177 Frederick Street

**COMMISSIONERS**

P. O. Box 88005  
Moreno Valley CA 92552

**Arthur Butler**  
Riverside

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW**

**Glen Holmes**  
Hemet

File No.: ZAP1110MA15

**John Lyon**  
Riverside

Related File No.: P15-003 (Revised Tentative Tract Map), PA15-0002 (Plot Plan)

APNs: 291-050-003, 291-050-004, 291-050-012, 291-050-013

**Greg Pettis**  
Cathedral City

Dear Ms. Descoteaux:

**Steve Manos**  
Lake Elsinore

Under the delegation of the Riverside County Airport Land Use Commission (ALUC), staff reviewed the above-referenced proposal to develop 273 apartment units in twenty buildings up to three stories in height, with a leasing office, recreation center, and pool, on 13.90 gross acres (12.06 net acres) located southerly of Box Springs Road, easterly of the southerly extension of Clark Street, and northerly of State Route 60 in the City of Moreno Valley. The Tentative Tract Map proposes to create a single legal parcel and facilitate the road dedications and improvements.

**STAFF**

**Director**  
**Ed Cooper**

John Guerin  
Russell Brady  
Barbara Santos

The site is located within Airport Compatibility Zone D of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone D in this AIA, land use intensity is not restricted.

County Administrative Center  
4080 Lemon St., 14th Floor.  
Riverside, CA 92501  
(951) 955-5132

The elevation of Runway 14-32 at its northerly terminus is approximately 1,535 feet above mean sea level (1535 feet AMSL). At a distance of approximately 17,900 feet from the runway to the property line, FAA review would be required for any structures with top of roof exceeding 1714 feet AMSL. The proposed maximum finished floor elevation is 1597.40 feet AMSL and the project includes proposed building elevations with a maximum height of 38.5 feet for a total maximum elevation of 1635.90 feet AMSL. Therefore, Federal Aviation Administration (FAA) obstruction evaluation review would not be required.

[www.rcaluc.org](http://www.rcaluc.org)

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, subject to the following conditions:

**CONDITIONS:**

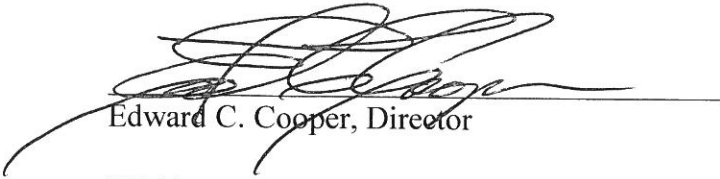
1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky.
2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area, including but not limited to, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, artificial marshes, incinerators, and fly ash disposal.
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached notice shall be provided to all potential purchasers and tenants, and shall be recorded as a deed notice.
4. Any new detention or retention basins on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention/retention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
5. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.

If you have any questions, please contact Russell Brady, Contract Planner, at (951) 955-0549, or John Guerin, Principal Planner, at (951) 955-0982.

**AIRPORT LAND USE COMMISSION**

**March 2, 2015**

Sincerely,  
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Edward C. Cooper, Director

RB:bks

Attachments: Notice of Airport in Vicinity

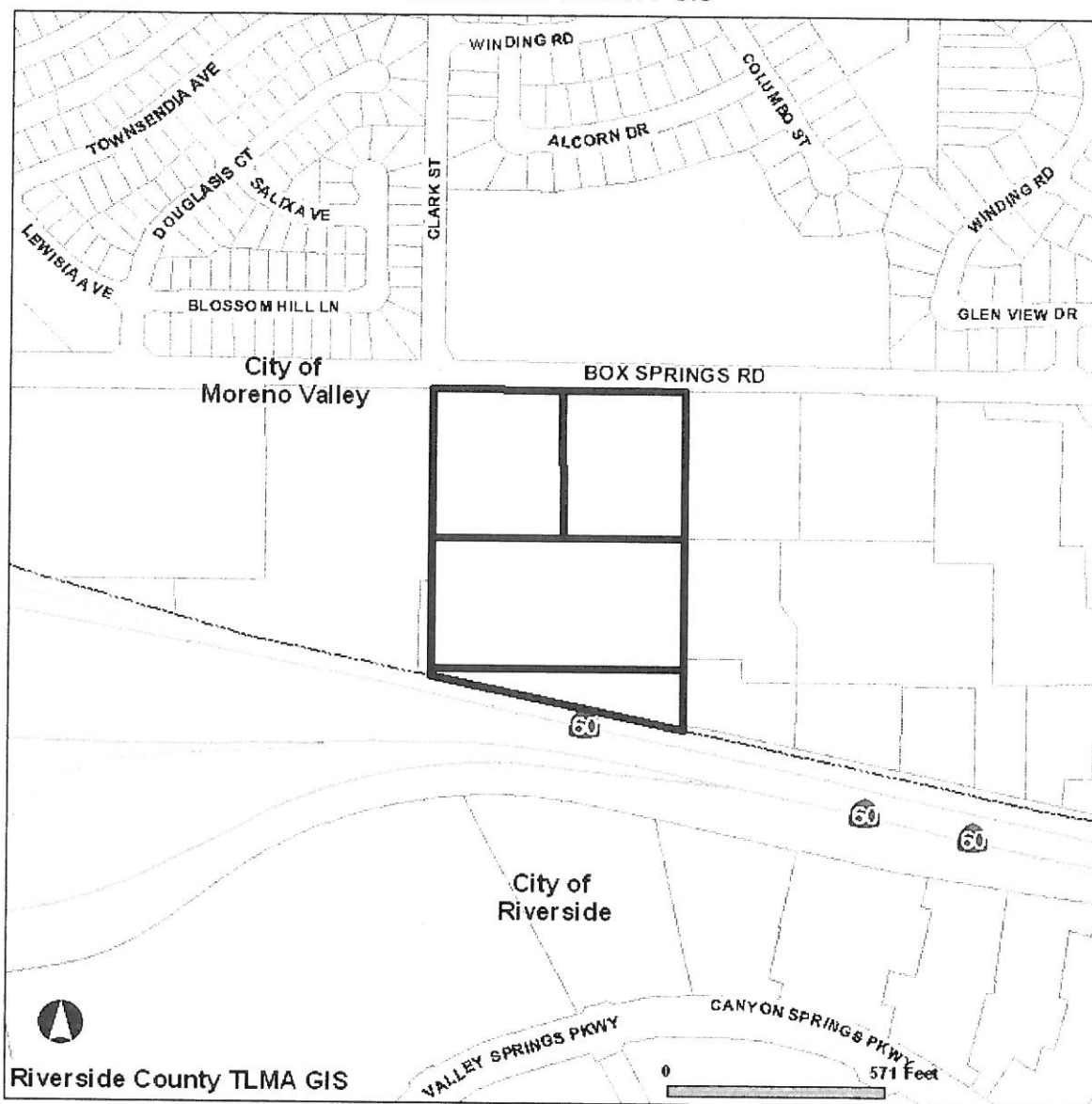
cc: Oak Parc Partners, LP (applicant/payee)  
Trip Hord (representative)  
Garry Brown, Trustee (landowner)  
Gary Gosliga, Airport Manager, March Inland Port Airport Authority  
Denise Hauser or Sonia Pierce, March Air Reserve Base  
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1110MA15\ZAP1110MA15.LTR.doc

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

RIVERSIDE COUNTY GIS



**Selected parcel(s):**

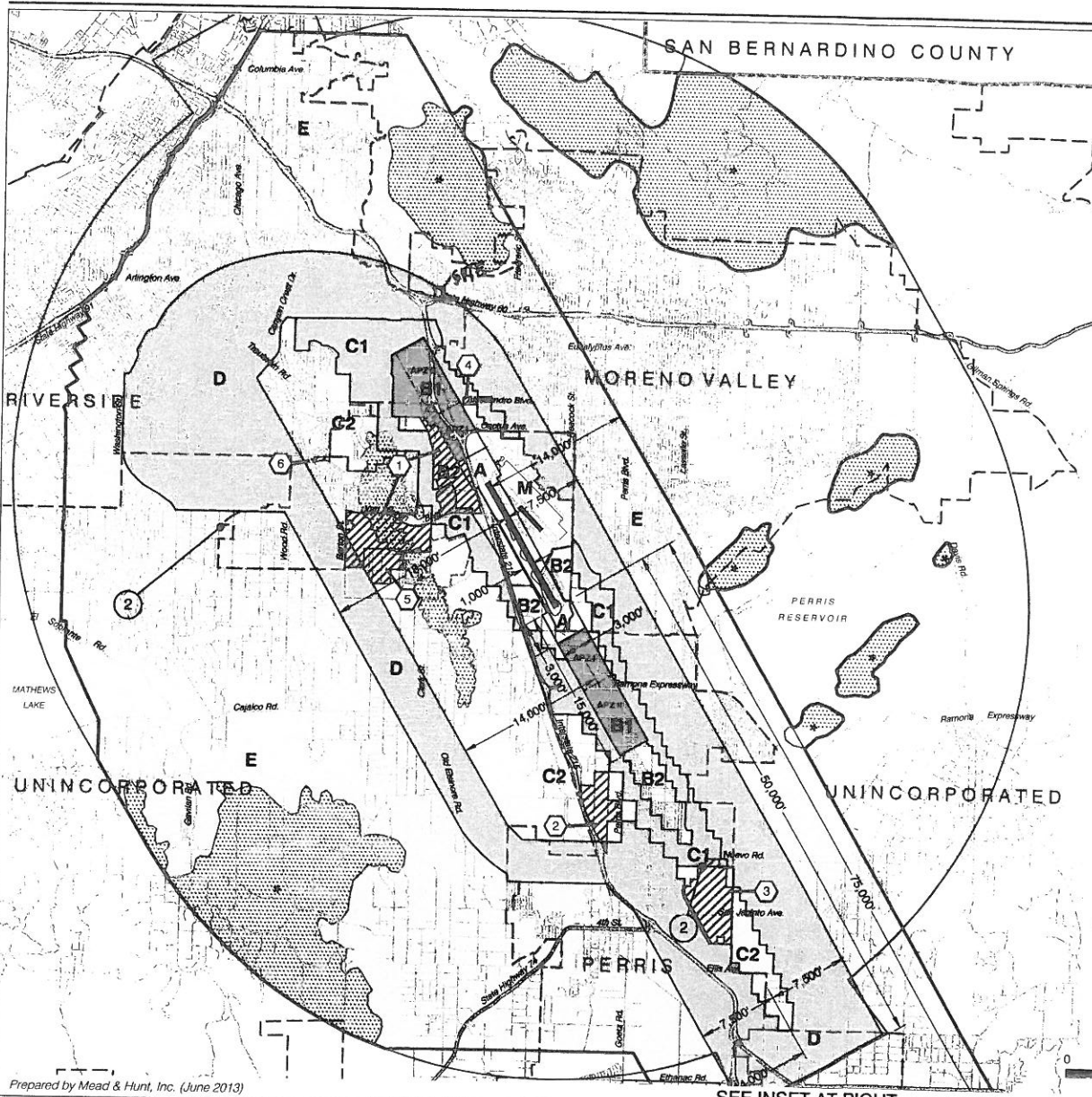
291-050-003 291-050-004 291-050-012 291-050-013

**\*IMPORTANT\***

Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON...Wed Feb 25 17:25:51 2015

Version 131127



**LEGEND**

**Compatibility Zones**

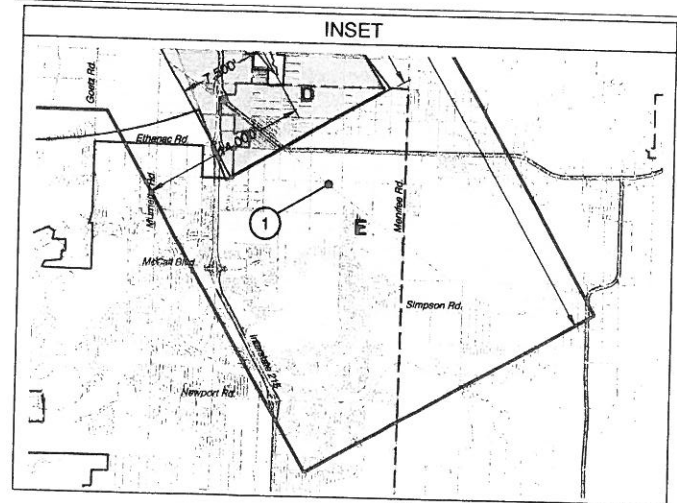
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

**Boundary Lines**

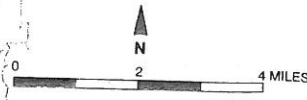
- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- ▨ Site-Specific Exceptions (existing local agency commitments to development projects)

- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



Note:  
All dimensions are measured from runway ends and centerlines.



Base map source: County of Riverside 2013

**Riverside County  
Airport Land Use Commission  
March Air Reserve Base / Inland Port Airport  
Land Use Compatibility Plan  
(Adopted November 13, 2014)**

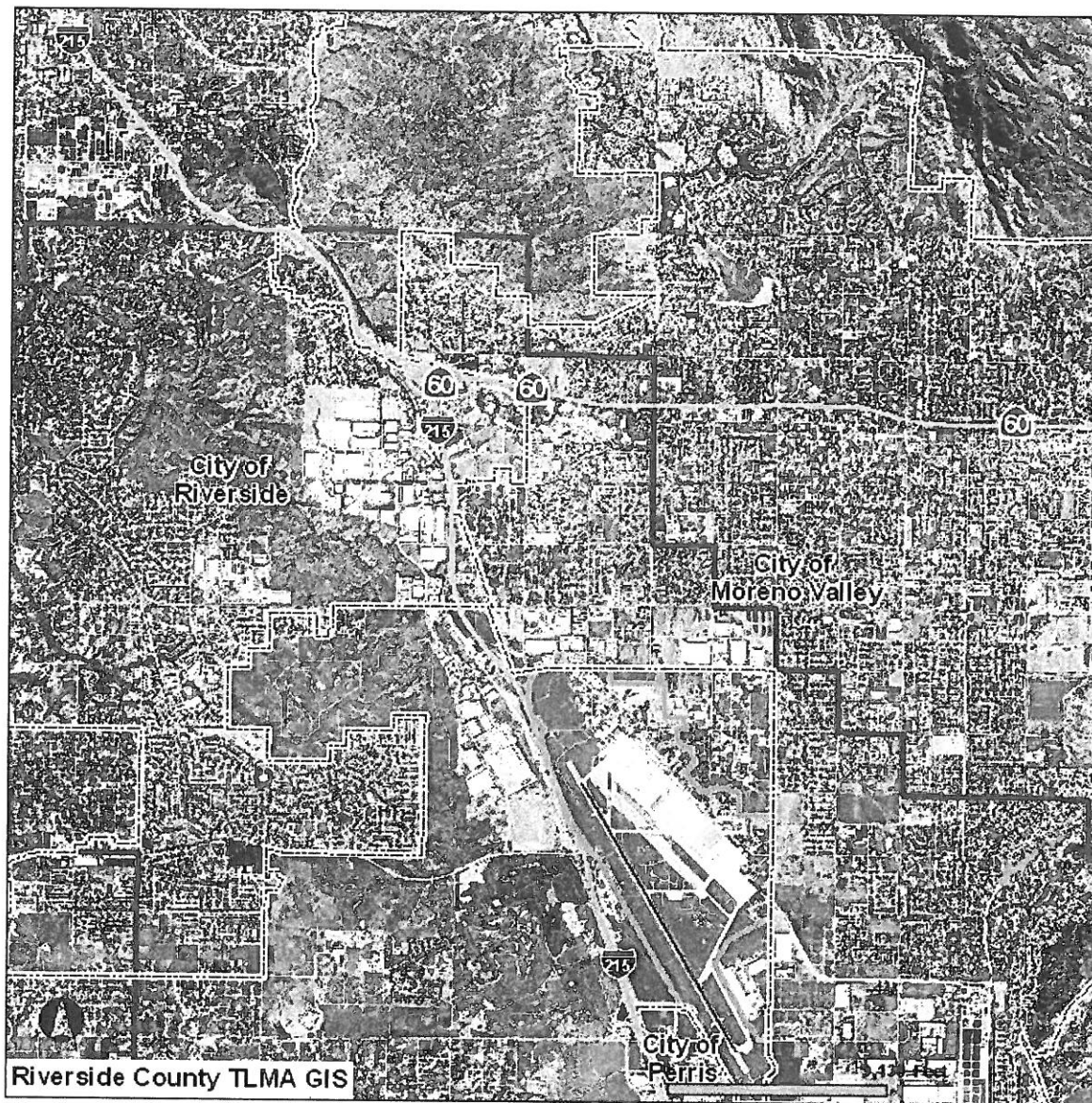
**Map MA-1  
Compatibility Map  
March Air Reserve Base / Inland Port Airport**

X:\MAPS\1014827\G1\CH3\MA1\compatibility\_2013.dwg Des. 10, 2014, 1:18pm

Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

RIVERSIDE COUNTY GIS



**Selected parcel(s):**

291-050-003 291-050-004 291-050-012 291-050-013

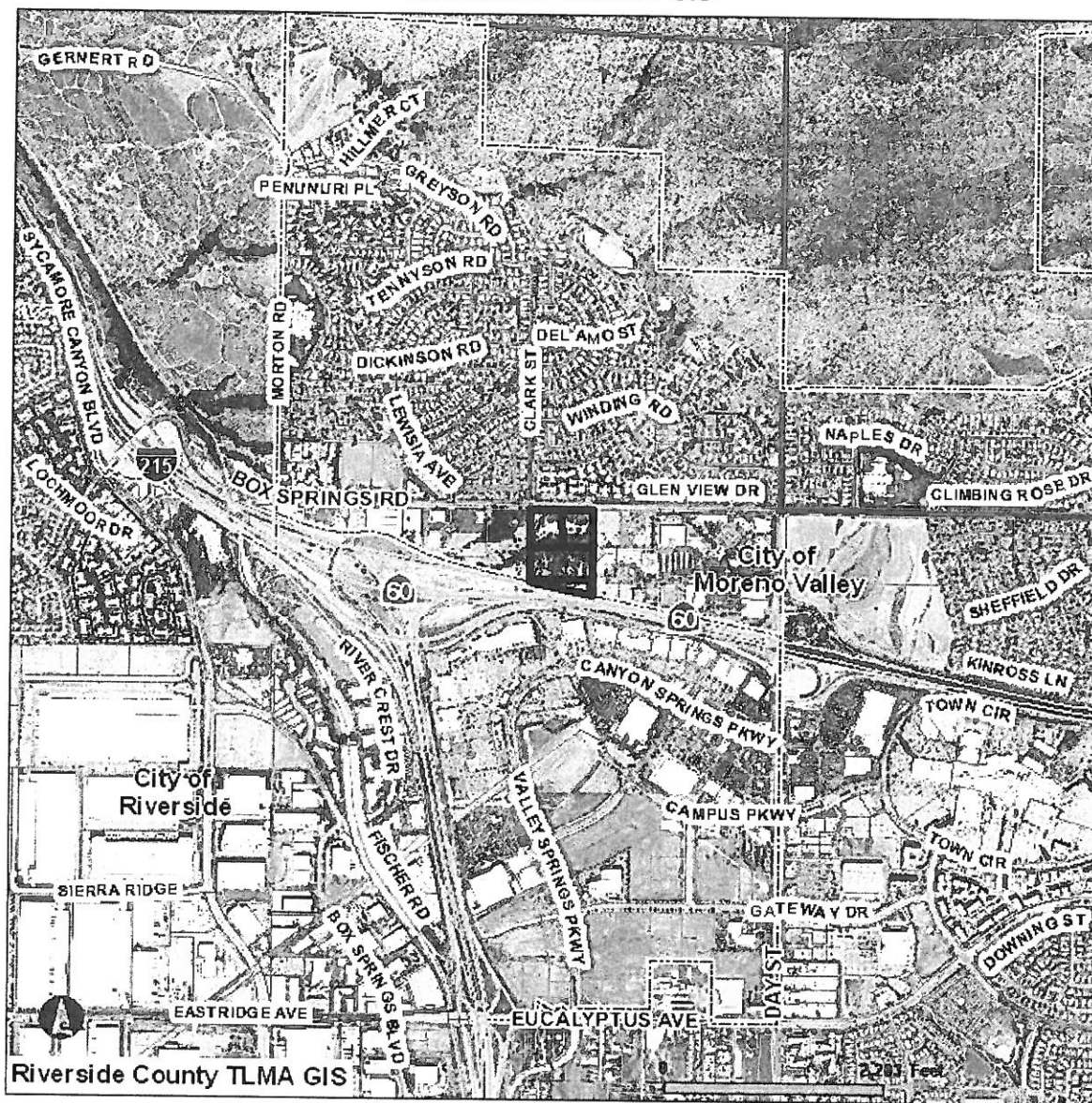
**\*IMPORTANT\***

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Version 131127

RIVERSIDE COUNTY GIS



**Selected parcel(s):**

291-050-003 291-050-004 291-050-012 291-050-013

**\*IMPORTANT\***

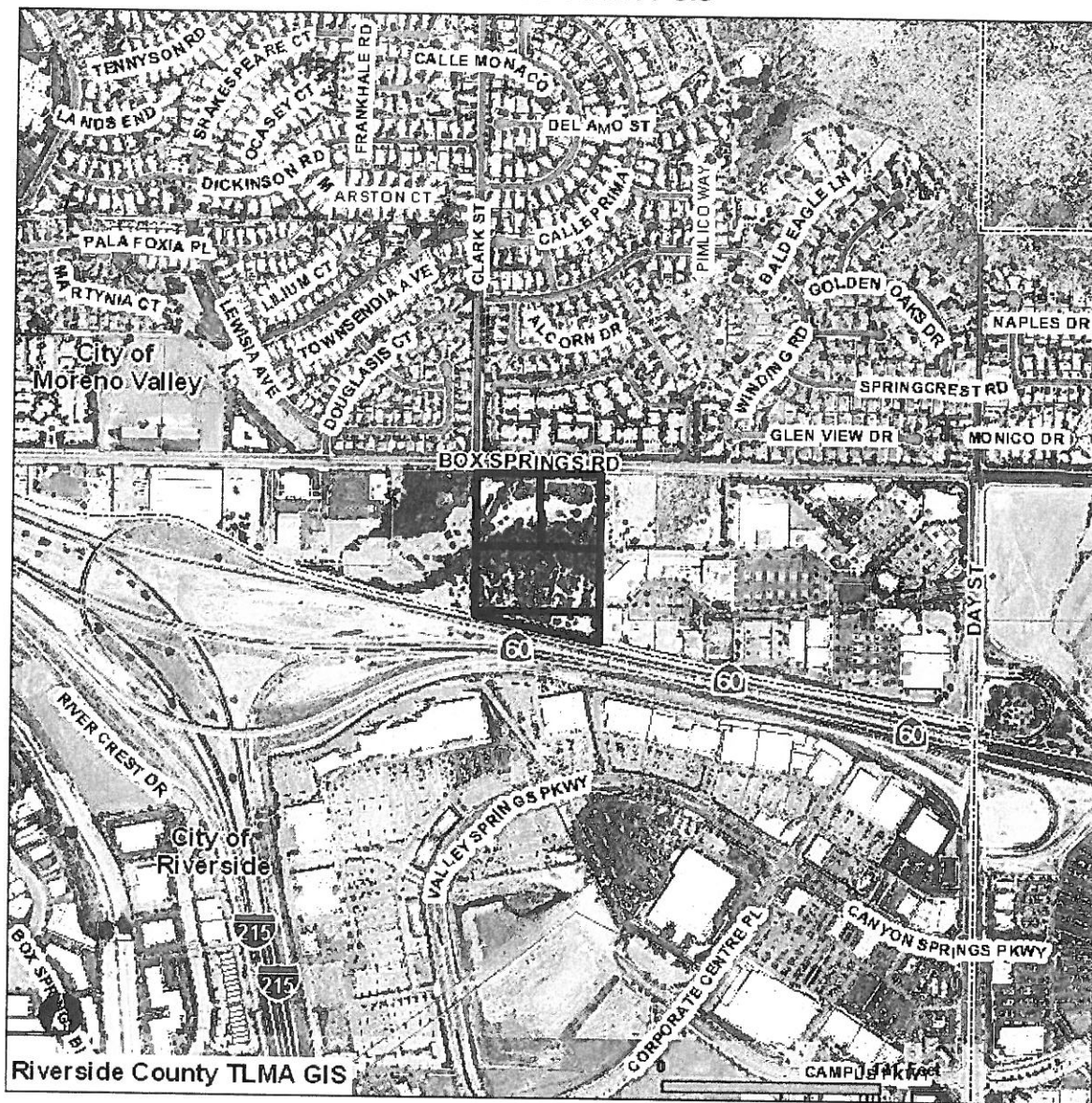
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REPORT PRINTED ON...Wed Feb 25 17:33:32 2015

Version 131127



RIVERSIDE COUNTY GIS



Selected parcel(s):

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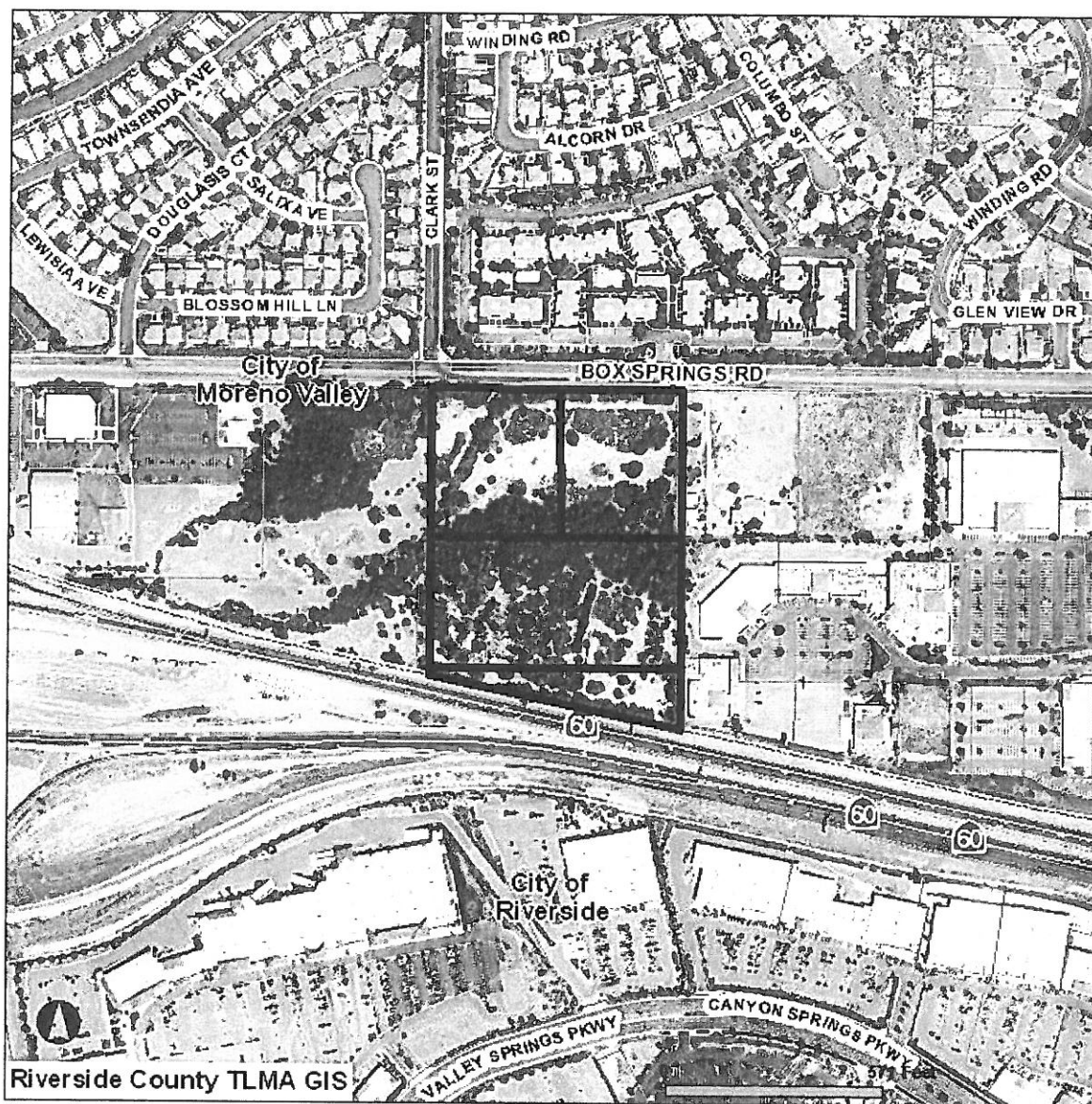
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REPORT PRINTED ON... Wed Feb 25 17:34:14 2015

Version 131127

RIVERSIDE COUNTY GIS



**Selected parcel(s):**

291-050-003 291-050-004 291-050-012 291-050-013

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REPORT PRINTED ON...Wed Feb 25 17:28:57 2015

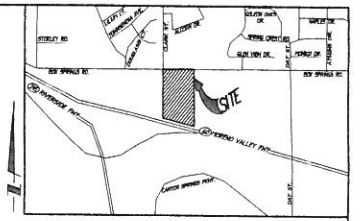
Version 131127

# PLOT PLAN

## REVISED TTM. 35414 FOR CONDOMINIUM PURPOSES

### CITY OF MORENO VALLEY

#### FEBRUARY 2015

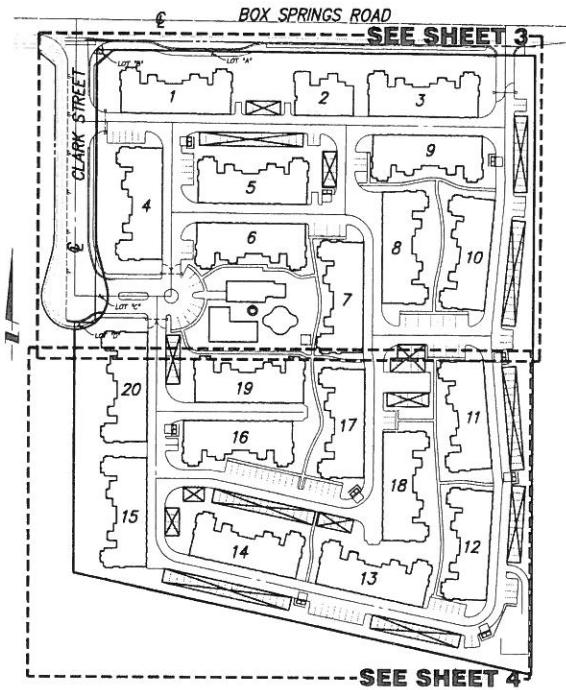


#### ON-SITE CONSTRUCTION

1. CONSTRUCT 4" CURB ONLY.
2. CONSTRUCT 3" WIDE RIBBON GUTTER.
3. CONSTRUCT RETAINING WALL 8" MAX.
4. CONSTRUCT TRASH ENCLOSURES.
5. CONSTRUCT COVERED PARKING STRUCTURE.
6. INSTALL A.C. PAVING.
7. INSTALL CONCRETE HARDSCAPE.
8. INSTALL PERVIOUS PAVERS.
9. INSTALL 2'x2' GRATED INLET.
10. INSTALL 4" CATCH BASIN.
11. INSTALL HDPE S.D. PIPE.
12. INSTALL BIO-RETENTION AREA SEE DETAIL SHEET 2.
13. INSTALL 3/4" RCP S.D. PIPE AND 26" WIDE PUBLIC S.D. EASEMENT.
14. INSTALL EARTHEN CHANNEL.
15. INSTALL HEADWALL.

#### OFF-SITE CONSTRUCTION

16. CONSTRUCT 4" CURB AND GUTTER PER CITY OF MORENO VALLEY STD. DWG. MYSH-20A-0.
17. CONSTRUCT 6" WIDE CONCRETE SIDEWALK PER CITY OF MORENO VALLEY STD. DWG. MYSH-18A-0.
18. CONSTRUCT DRIVEWAY APPROACH PER CITY OF MORENO VALLEY STD. DWG. MYSH-10A-0.
19. CONSTRUCT ACCESS RAMPS PER CITY OF MORENO VALLEY STD. DWG. MYSH-14A-0.
20. CONSTRUCT CROSS GUTTER PER CITY OF MORENO VALLEY STD. DWG. MYSH-127-0.
21. CONSTRUCT BUS TURNOUT PER CITY OF MORENO VALLEY STD. DWG. MYSH-91-0.
22. INSTALL A.C. PAVING PER CITY OF MORENO VALLEY STD. AND RECOMMENDATIONS OF SOils REPORT (SECTION TO BE VERIFIED).
23. INSTALL PARKWAY LANDSCAPING.
24. INSTALL STREET LIGHT PER CITY OF MORENO VALLEY STD. DWG. MALT-80A-0.
25. INSTALL CONCRETE COLLAR PER CITY OF MORENO VALLEY STD. DWG. MYFE-340-0.



#### LEGEND

- EXISTING PROPERTY LINE
- EXISTING RIGHT OF WAY
- EXISTING CENTERLINE
- PROPOSED CURB
- EXISTING CURB
- PROPOSED SIDEWALK
- EXISTING SIDEWALK
- EXISTING DIRT ROAD
- PROPOSED PARKING STRIPE
- EXISTING EASEMENT
- EXISTING CONTOUR MAJOR
- EXISTING CONTOUR MINOR
- EXISTING TRAIL
- EXISTING BUILDING
- EXISTING CONCRETE
- EXISTING EDGE OF PAVEMENT
- PROPOSED ELEVATION
- EXISTING CONTOUR ELEVATION
- EXISTING SPOT ELEVATION

#### LEGEND

- AC PAVING
- CONCRETE PAVING
- PERVIOUS CONCRETE/PAVERS
- LANDSCAPE BIORETENTION AREA

#### SUBDIVISION INFORMATION

**PROPOSED TRACT INFORMATION:**  
 PROPOSED TRACT 35414 IS A 273 UNIT APARTMENT PROJECT, ON BOX SPRINGS ROAD ELY OF CLARK STREET AND SLY OF BOX SPRINGS ROAD. THE EXISTING PARCEL IS ZONED R-30 AND THE EXISTING LAND USE IS VACANT. THE SITE WILL CONNECT TO EXISTING PUBLIC UTILITIES LOCATED IN BOX SPRINGS ROAD AND CLARK STREET.

**WATER SUPPLY INFORMATION:**  
 THE PROPOSED TRACT WILL BE SERVED BY CONNECTING TO THE EXISTING 12" WATER MAIN IN CLARK STREET. THE PROPOSED TRACT WILL REQUIRE AN AVERAGE DAILY WATER DEMAND OF 19,178 GPD (E.G. 90 GALLONS A DAY X 2.3 PEOPLE PER UNIT X 273 UNITS).

**SEWAGE DISPOSAL:**  
 THE PROPOSED TRACT WILL BE CONNECTED TO THE CITY'S EXISTING SEWAGE NETWORK BY CONNECTING TO THE SEWER IN BOX SPRINGS ROAD. THE PROPOSED DEVELOPMENT IS ANTICIPATED TO REQUIRE AN AVERAGE DAILY SEWER DEMAND OF 16,090 GPD (E.G. 100 GALLONS A DAY X 2.3 PEOPLE PER UNIT X 273 UNITS).

**STORM DRAIN SYSTEM:**  
 THERE ARE OFF-SITE TRIBUTARY FLOWS COMING AT THIS SITE, IN THE FORM OF AN EXISTING 30" RCP THAT OUTLETS AT THE EASTERLY PROPERTY LINE. THIS TRACT PROPOSES A 36" RCP TO CARRY THE EXISTING FLOWS THROUGH THE SITE. ALL ON-SITE FLOWS WILL BE TREATED BY THE PROJECT SPECIFIC WATER QUALITY MANAGEMENT PLAN PRIOR TO RELEASING FLOWS INTO THE NATURAL DRAINAGE PATH TO THE WESTERLY PROPERTY. THERE ARE NO SPECIAL FLOOD HAZARDS ON THIS SITE.

**STREET IMPROVEMENTS:**  
 THE DEVELOPMENT PROPOSES TO INSTALL PAVEMENT, SIDEWALK, ACCESS RAMPS, FIRE HYDRANTS, STREETLIGHTS AND LANDSCAPING ALONG BOX SPRINGS ROAD AND CLARK STREET. ALL OTHER UTILITIES (T.V. CABLE, PHONE, ELECTRICAL AND GAS) WILL BE PLACED UNDERGROUND WITHIN THE PROPOSED PUBLIC STREET.

**PROTECTIVE COVENANTS AND ASSOCIATIONS:**  
 THE PROPERTY OWNER WILL BE RESPONSIBLE FOR MAINTENANCE OF COMMON AREAS WITHIN THIS DEVELOPMENT.

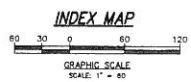
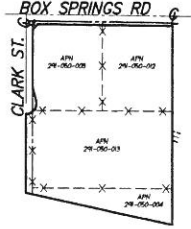
**PHASING:**  
 THERE IS NO PROPOSED PHASING OF THIS SITE AT THE PRESENT TIME.

UNIT TYPE	RATIO	NUMBER OF UNITS	PARKING
AP1	1.0	39	39
AP2	2.0	87	174
AP3	2.0	69	138
<b>TOTAL REQUIRED</b>			<b>351</b>

UNIT TYPE	RATIO	NUMBER OF UNITS	PARKING
AP1	1.0	39	39
AP2	1.0	87	87
AP3	1.0	69	69
<b>TOTAL REQUIRED</b>			<b>195</b>

PARKING TYPE	PROPOSED PARKING
GARAGE	40
OPEN	95
<b>TOTAL</b>	<b>135</b>

LOT LETTER	S.F.	DESCRIPTION
A	363	SUB. C/UT. BACK
B	26	SUB. C/UT. BACK
C	708	SEALED (O.D.)
D	25	SEALED (O.D.)



NO WORK SHALL BE DONE ON THIS SITE UNTIL BELOW AGENCY IS NOTIFIED BY NOTIFICATION TO GRADE OR EXCAVATE Underground Service Alert

Call Toll Free 1-800-277-8000

WWW.CALLERT.COM

THE WORKING DAYS BEFORE THE DAY

**BENCHMARK**  
 CITY OF RIVERSIDE: 05-48  
 2 1/2" BRASS DISK STAMPED  
 "CITY OF RIVERSIDE: 05-48 STATION  
 64 1591 CALM" SET IN THE TOP  
 OF A CATCH BASIN ALONG THE  
 NORTHERLY SIDE OF CLAYTON  
 SPRINGS TRKY 0.3 MILES WEST  
 OF DAY STREET.

DATE: 1988 ELEV: 1586.754

BASIS OF BEARING	REVIEW BY CITY STAFF	MARK	DATE	INITIAL	DESCRIPTION	REC.	APPR.	DATE
BASIS OF BEARINGS IS THE CENTERLINE OF BOX SPRINGS ROAD, TAKEN AS NORTH.	OFFICE ENGINEERING DIVISION MANAGER							
BY STATION EAST FOR TRACT 17176	LAND DEVELOPMENT							
BOOK 124 PAGES 54-58	PLANNING							
INCLUDES OF MAPS, RECORDS OF RIVERSIDE COUNTY, CALIFORNIA.	TRANSPORTATION							
	PARKS AND COMMUNITY SERVICES							
	SPECIAL DISTRICTS							
	STORM WATER MANAGEMENT PRGM							

RECOMMENDED:	DATE
DEPUTY PW DIR/ASSISTANT CITY ENGINEER R.C.E. #32483	
APPROVED:	DATE
AHMAD R. ANSARI PUBLIC WORKS DIRECTOR/CITY ENGINEER R.C.E. #33318	

**PSOMAS**  
 1500 IOWA AVENUE, SUITE 210  
 RIVERSIDE, CA 92507  
 (951) 787-8421 WWW.PSOMAS.COM

UNDER THE SUPERVISION OF:  
 STEVEN B. FRELSON  
 R.C.E. #2110 EXP: 03/21/18

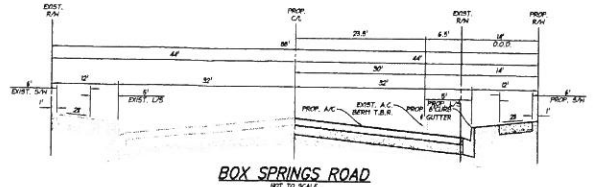
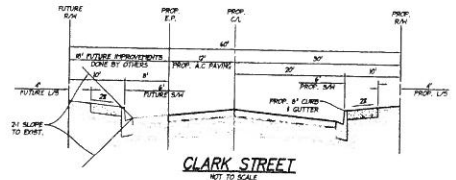
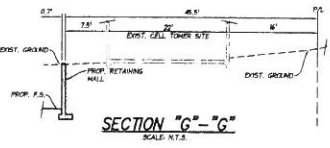
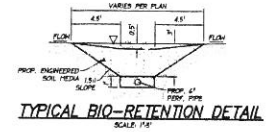
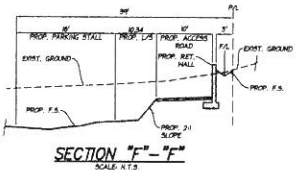
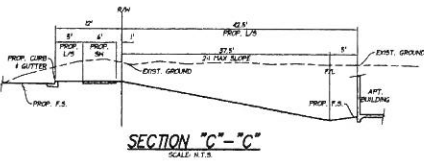
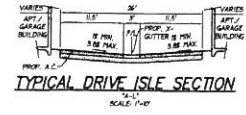
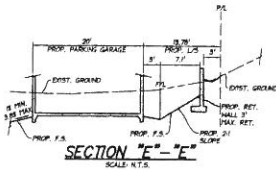
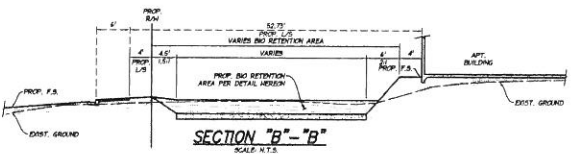
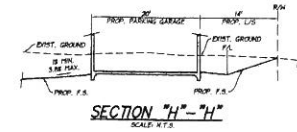
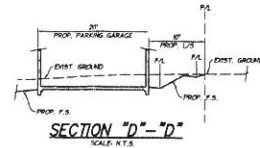
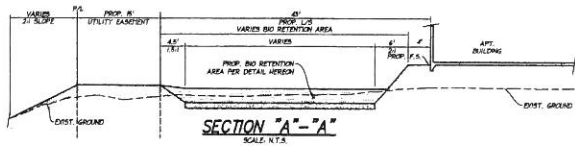
CITY OF MORENO VALLEY

## PLOT PLAN

### REVISED TRACT NO. 35414

### TITLE SHEET

SHEET 1 OF 4



NO WORK SHALL BE DONE ON THIS SITE UNTIL BELOW AGENCY IS NOTIFIED OF INTENTION TO GRADE OR EXCAVATE.

**Underground Service Alert**

Call Toll Free: 1-800-277-6100

WWW.CALLIT.COM

THE WORKING DATE BEFORE YOU DIG.

**BENCHMARK**

CITY OF RIVERSIDE: OPS-66

2 1/2" BRASS DISK STAMPED

TOTY OF RIVERSIDE OPS STATION

66 1891 CALMAG SET IN THE TOP

89'31/4" CAST IRON TRACT 17176

BOOK 126 PAGES 54-56

INCLUSIVE OF MARK RECORDS OF

RIVERSIDE COUNTY, CALIFORNIA

SPRINGS PWAY 0.3 MILES WEST

OF DAY STREET.

DATA: 1988 ELEV 1586.754

BASIS OF BEARING		REVIEW BY CITY STAFF		OFFICE		ENGINEERING DIVISION MANAGER		LAND DEVELOPMENT		PLANNING		TRANSPORTATION		PARKS AND COMMUNITY SERVICES		SPECIAL DISTRICTS		STORM WATER MANAGEMENT PROGRAM		
MARK	DATE	INITIAL	DATE	MARK	DATE	INITIAL	DATE	DESCRIPTION	REC.	APPR.	DATE	MARK	DATE	INITIAL	DATE	DESCRIPTION	REC.	APPR.	DATE	

**CITY OF MORENO VALLEY APPROVALS**

RECOMMENDED:

PREM KUMAR DATE

DEPUTY PW DIR/ASSISTANT CITY ENGINEER

R.C.E. #C52483

APPROVED:

AMAD R. ANSARI DATE

PUBLIC WORKS DIRECTOR/CITY ENGINEER

R.C.E. #C51518



**PSOMAS**

1500 IOWA AVENUE, SUITE 210

RIVERSIDE, CA 92507

(951) 787-8421 WWW.PSOMAS.COM

UNDER THE SUPERVISION OF:

STEVEN B. FINSON DATE

R.C.E. 42110 EXP: 03/31/16

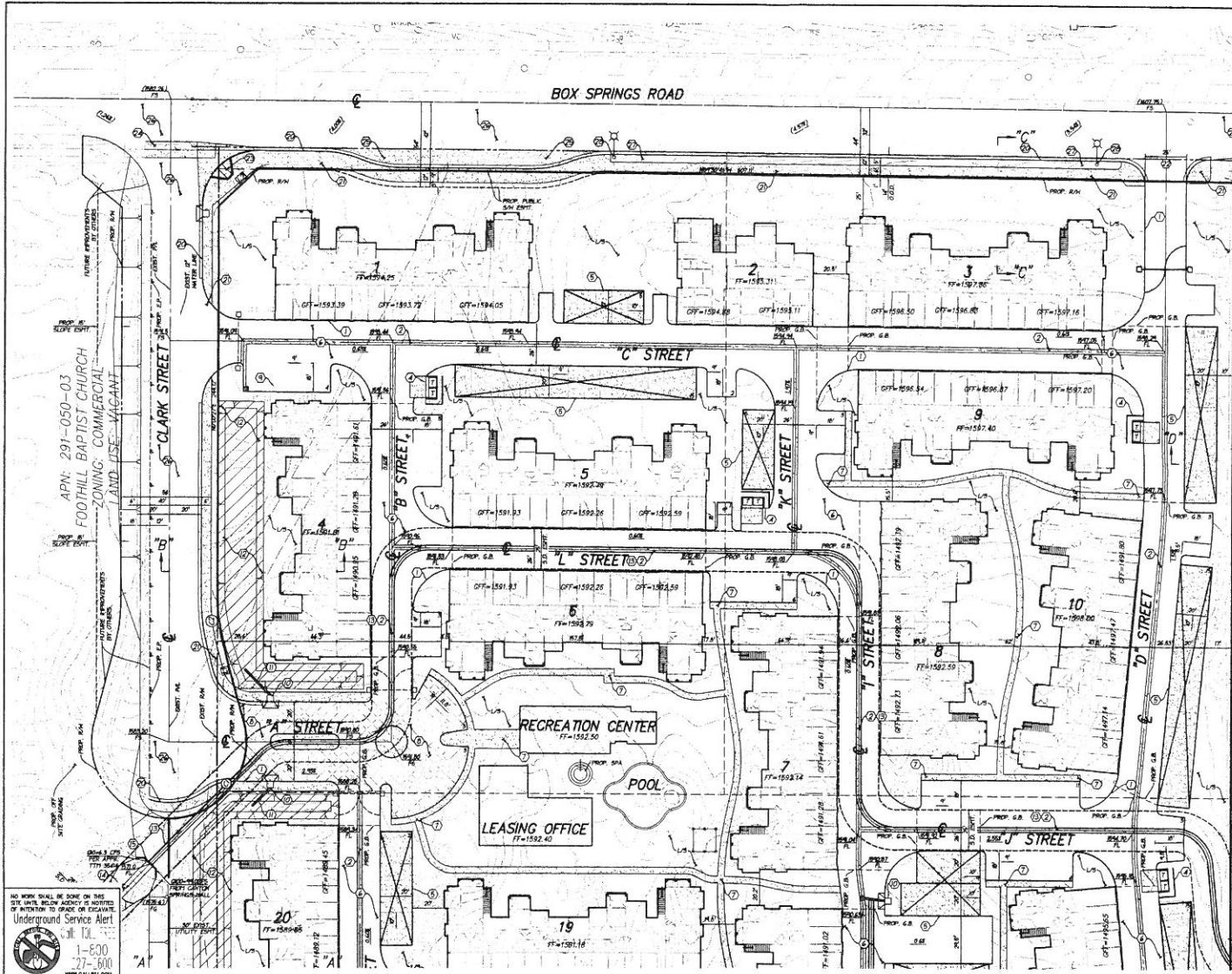
**CITY OF MORENO VALLEY**

**PLOT PLAN**

**REVISED TRACT NO. 35414**

**SECTIONS AND DETAILS**

SHEET 2 OF 4



APN: 291-050-002  
 JAMES GRANTON AND  
 EDDIE MAE GRANTON  
 ZONING: COMMERCIAL  
 LAND USE: EXISTING SFD

**ON-SITE CONSTRUCTION**

- ① CONSTRUCT 6" CURB ONLY.
- ② CONSTRUCT 3" WIDE RIBBON GUTTER.
- ③ CONSTRUCT RETAINING WALL 6" MAX.
- ④ CONSTRUCT TRASH ENCLOSURES.
- ⑤ CONSTRUCT COVERED PARKING STRUCTURE.
- ⑥ INSTALL A.C. PAVING.
- ⑦ INSTALL CONCRETE HARDSCAPE.
- ⑧ INSTALL PERVIOUS PAVERS.
- ⑨ INSTALL 2X2" GRATED INLET.
- ⑩ INSTALL 4" CATCH BASIN.
- ⑪ INSTALL HDPE S.D. PIPE.
- ⑫ INSTALL BIO-RETENTION AREA SEE DETAIL SHEET 2.
- ⑬ INSTALL 36" R.O.P. S.D. PIPE AND 24" WIDE PUBLIC S.D. EASEMENT.
- ⑭ INSTALL EARTHEN CHANNEL.
- ⑮ INSTALL HEADWALL.

**OFF-SITE CONSTRUCTION**

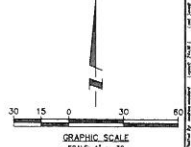
- ⑯ CONSTRUCT 6" CURB AND GUTTER PER CITY OF MORENO VALLEY STD. DWG. PMS-100A-0.
- ⑰ CONSTRUCT 6" WIDE CONCRETE SIDEWALK PER CITY OF MORENO VALLEY STD. DWG. PMS-100A-0.
- ⑱ CONSTRUCT DRIVEWAY APPROACH PER CITY OF MORENO VALLEY STD. DWG. PMS-100A-0.
- ⑲ CONSTRUCT ACCESS RAMP PER CITY OF MORENO VALLEY STD. DWG. PMS-100A-0.
- ⑳ CONSTRUCT CROSS GUTTER PER CITY OF MORENO VALLEY STD. DWG. PMS-107-0.
- ㉑ CONSTRUCT BUS TURNOUT PER CITY OF MORENO VALLEY STD. DWG. PMS-100A-0.
- ㉒ INSTALL A.C. PAVING PER CITY OF MORENO VALLEY STD. AND RECOMMENDATIONS OF SOILS REPORT (SECTION TO BE VERIFIED).
- ㉓ INSTALL PARCHAY LANDSCAPING.
- ㉔ INSTALL STREET LIGHT PER CITY OF MORENO VALLEY STD. DWG. PMS-100A-0.
- ㉕ INSTALL CONCRETE COLLAR PER CITY OF MORENO VALLEY STD. DWG. PMS-100A-0.

**LINE TABLE**

LINE #	BEARING	DISTANCE
L1	S41° 54' 26" W	41.91
L2	S42° 20' 25" W	33.46

**CURVE TABLE**

CURVE #	LENGTH	RADIUS	DELTA	TAN
C1	87.90'	46.00'	109°25'24"	43.21'
C2	26.30'	100.41'	107°24'12"	13.26'



NO WORK SHALL BE DONE ON THIS SITE UNTIL A FIELD AGENCY IS ADVISED OF INTENTION TO GRADE OR EXCAVATE. Underground Service Alert Call Toll 1-800-477-9000 WWW.CALLTOLL.COM THE WORKING DAYS BEFORE THE DAY

**BENCHMARK**  
 CITY OF RIVERSIDE GPS-86  
 2 1/2" BRASS DISK STAMPED  
 CITY OF RIVERSIDE GPS STA 10N  
 66.1501 CORNER SET ON THE TOP  
 OF A CATCH BASIN ALONG THE  
 NORTHERLY SIDE OF CANTON  
 SPRINGS HWY 0.3 MILES WEST  
 OF DAY STREET.  
 DATUM 1988 ELEV 1536.754

BASIS OF BEARING	REVIEW BY CITY STAFF																					
BASIS OF BEARING IS THE CENTERLINE OF BOX SPRINGS ROAD, TAKEN AS NORTH. SECTION EAST BOX TRACT 17176, BLOCK 176 PAGES 54-58. INCLUSIVE OF MAPS, RECORDS OF RIVERSIDE COUNTY, CALIFORNIA.	OFFICE ENGINEERING DIVISION MANAGER LAND DEVELOPMENT PLANNING TRANSPORTATION PARKS AND COMMUNITY SERVICES SPECIAL DISTRICTS STORM WATER MANAGEMENT PRGM																					
	<table border="1"> <thead> <tr> <th>MARK</th> <th>DATE</th> <th>INITIAL</th> <th>DESCRIPTION</th> <th>REC.</th> <th>APPR.</th> <th>DATE</th> </tr> </thead> <tbody> <tr> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> </tbody> </table>	MARK	DATE	INITIAL	DESCRIPTION	REC.	APPR.	DATE														
MARK	DATE	INITIAL	DESCRIPTION	REC.	APPR.	DATE																

**CITY OF MORENO VALLEY APPROVALS**

RECOMMENDED: \_\_\_\_\_ DATE \_\_\_\_\_  
 DEPUTY PW DEPARTMENT CITY ENGINEER  
 R.C.E. #52463

APPROVED: \_\_\_\_\_ DATE \_\_\_\_\_  
 ANWAR B. ANSARI  
 PUBLIC WORKS DIRECTOR/CITY ENGINEER  
 R.C.E. #35316

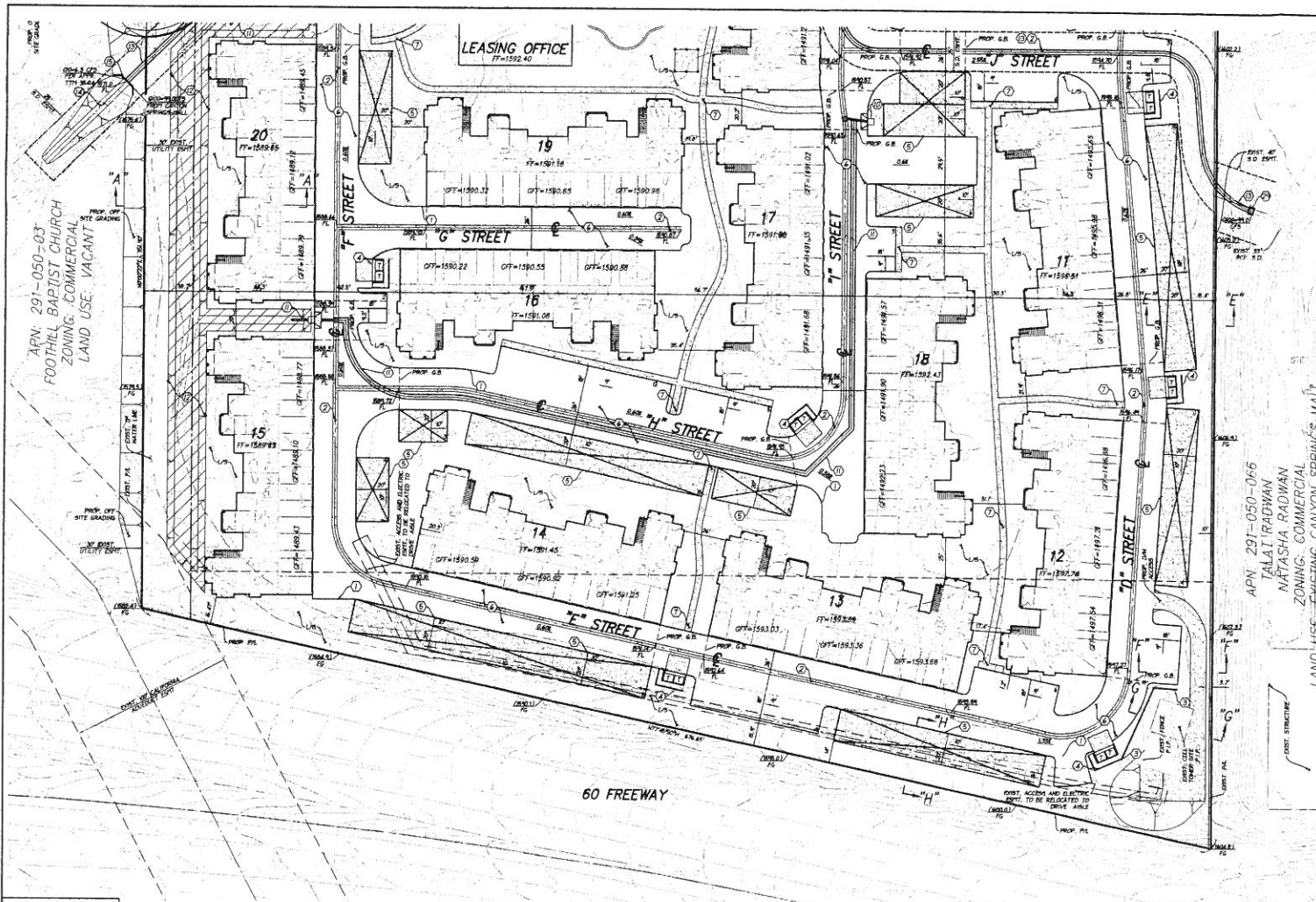


**PSOMAS**  
 1500 IOWA AVENUE, SUITE 210  
 RIVERSIDE, CA 92507  
 (951) 787-8421 WWW.PSOMAS.COM  
 UNDER THE SUPERVISION OF:  
 STEVEN B. FREDSON  
 R.C.E. #2110 EXP. 02/31/16

**CITY OF MORENO VALLEY**

**PLOT PLAN**  
 REVISED TRACT NO. 35414  
 CONCEPTUAL GRADING PLAN

SHEET 3 of 4



APN: 291-050-03  
 FOOTHILL BAPTIST CHURCH  
 ZONING: COMMERCIAL  
 LAND USE: VACANT

APN: 291-050-066  
 TALLI RADWAN  
 NATASHA RADWAN  
 ZONING: COMMERCIAL  
 LAND USE: EXISTING CANYON SPRINGS-MALL

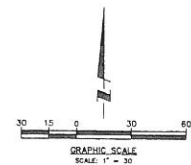
- ON-SITE CONSTRUCTION**
1. CONSTRUCT 4" CURB ONLY.
  2. CONSTRUCT 3" WIDE RUBBER GUTTER.
  3. CONSTRUCT RETAINING WALL 8" MAX.
  4. CONSTRUCT TRASH ENCLOSURES.
  5. CONSTRUCT COVERED PARKING STRUCTURE.
  6. INSTALL A.C. PAVING.
  7. INSTALL CONCRETE HARDSCAPE.
  8. INSTALL PERVIOUS PAVERS.
  9. INSTALL 2'x2' GRATED INLET.
  10. INSTALL 4" CATCH BASIN.
  11. INSTALL HOPE S.D. PIPE.
  12. INSTALL BIO-RETENTION AREA SEE DETAIL SHEET 2.
  13. INSTALL 3/4" RCP S.D. PIPE AND 25' WIDE PUBLIC S.D. EASEMENT.
  14. INSTALL EARTHEN CHANNEL.
  15. INSTALL HEADWALL.

- OFF-SITE CONSTRUCTION**
16. CONSTRUCT 4" CURB AND GUTTER PER CITY OF MORENO VALLEY STD. DNG. MVS1-100A-0.
  17. CONSTRUCT 4" WIDE CONCRETE SIDEWALK PER CITY OF MORENO VALLEY STD. DNG. MVS1-100A-0.
  18. CONSTRUCT DRIVEWAY APPROACH PER CITY OF MORENO VALLEY STD. DNG. MVS1-102A-0.
  19. CONSTRUCT ACCESS RAMP PER CITY OF MORENO VALLEY STD. DNG. MVS1-104A-0.
  20. CONSTRUCT CROSS GUTTER PER CITY OF MORENO VALLEY STD. DNG. MVS1-107-0.
  21. CONSTRUCT BUS TURNOUT PER CITY OF MORENO VALLEY STD. DNG. MVS1-161-0.
  22. INSTALL A.C. PAVING PER CITY OF MORENO VALLEY STD. AND RECOMMENDATIONS OF SOILS REPORT (SECTION TO BE VERIFIED).
  23. INSTALL PARKWAY LANDSCAPING.
  24. INSTALL STREET LIGHT PER CITY OF MORENO VALLEY STD. DNG. MVL1-400A-0.
  25. INSTALL CONCRETE COLLAR PER CITY OF MORENO VALLEY STD. DNG. MVE1-340-0.

LINE TABLE		
LINE #	BEARING	DISTANCE
L1	S47° 59' 28"E	44.49'
L2	S48° 20' 28"E	35.46'

CURVE TABLE				
CURVE #	LENGTH	RADIUS	DELTA	TAN
C1	47.96'	48.07'	109.92°	42.54'
C2	26.36'	18.67'	149.92°	18.28'



NO WORK SHALL BE DONE ON THIS SITE UNTIL THERE HAS BEEN AGENCY REVIEW OF INTENTION TO GRADE OR EXCAVATE  
**Underground Service Alert**  
 Call 811  
 1-800-477-6000  
 WWW.CALIFORNIA.GOV  
 TWO WORKING DAYS BEFORE YOU DIG

BENCHMARK	BASIS OF BEARING	REVIEW BY CITY STAFF		
CITY OF RIVERSIDE: GPS-66 2 1/2" BRASS DISK STAMPED CITY OF RIVERSIDE GPS STATION 66 1891 CALM - SET IN THE TOP OF A CATCH BASIN ALONG THE NORTHERLY SIDE OF CANYON SPRINGS HWY 0.3 MILES WEST OF 64 <sup>TH</sup> STREET.	BASIS OF BEARINGS IS THE CENTERLINE OF BOX SPRINGS ROAD, TAKEN AS NORTH 89°31'04" EAST PER TRACT 17176 BOOK 126 PAGES 54-58 INCLUDES OF MAPS, RECORDS OF RIVERSIDE COUNTY, CALIFORNIA	OFFICE ENGINEERING DIVISION MANAGER LAND DEVELOPMENT PLANNING TRANSPORTATION PARKS AND COMMUNITY SERVICES SPECIAL DISTRICTS STORM WATER MANAGEMENT PROGRAM	INITIAL	DATE
		MARK	DATE	INITIAL
		DESCRIPTION	REC.	APPR.
		REVISION		DATE

**CITY OF MORENO VALLEY APPROVALS**

RECOMMENDED:  
 BREW RYANAR DATE  
 DEPUTY PW DIR/ASSISTANT CITY ENGINEER  
 R.C.E. #252463

APPROVED:  
 AHMAD B. ANSARI DATE  
 PUBLIC WORKS DIRECTOR/CITY ENGINEER  
 R.C.E. #251318

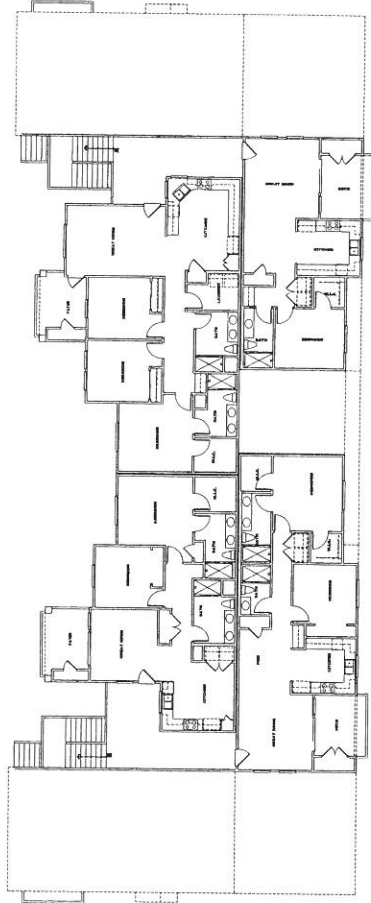


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 STEVEN B. INGHOS  
 R.C.E. #2110 EXP: 03/31/16

**CITY OF MORENO VALLEY**

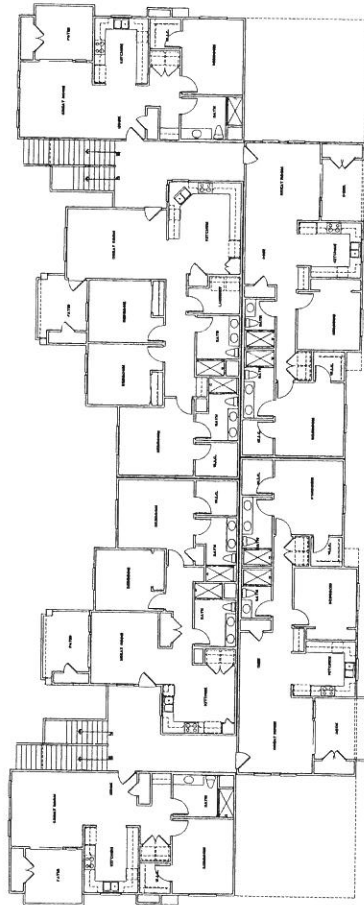
**PLOT PLAN**  
 REVISED TRACT NO. 35414  
 CONCEPTUAL GRADING PLAN

SHEET 4 OF 4



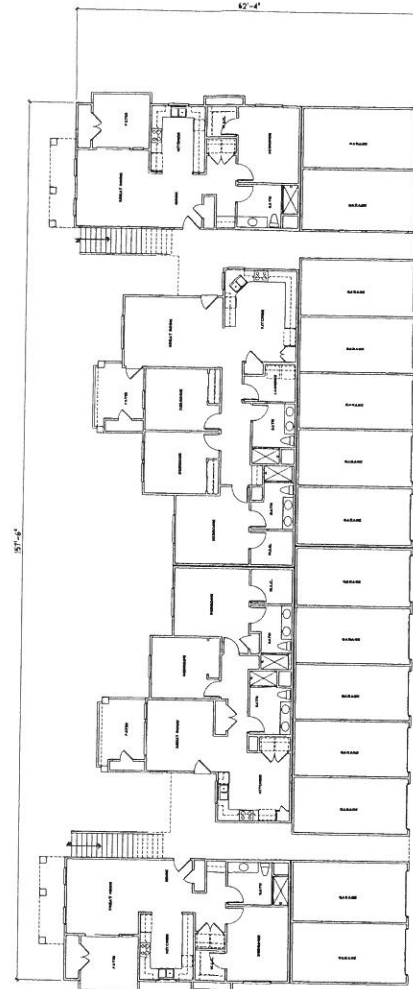
**THIRD FLOOR PLAN**

- SUMMARY**
- 1 BEDROOM - 1 UNITS
  - 2 BEDROOM - 2 UNIT
  - 3 BEDROOM - 1 UNIT



**SECOND FLOOR PLAN**

- SUMMARY**
- 1 BEDROOM - 1 UNITS
  - 2 BEDROOM - 4 UNIT
  - 3 BEDROOM - 1 UNIT



**FIRST FLOOR PLAN**

- SUMMARY**
- 1 BEDROOM - 2 UNITS
  - 2 BEDROOM - 1 UNIT
  - 3 BEDROOM - 1 UNIT

MR. PAUL REIM  
 OAK PARC PARTNERS, L.P.  
 4101 BIRCH ST. STE. 150  
 NEWPORT BEACH, CA 92669

**CONCEPT BUILDING PLANS**



Architecture

8111 E. Santa Ana Canyon Road  
 Suite 300-313  
 Anaheim, CA 92808  
 (714) 431-1118

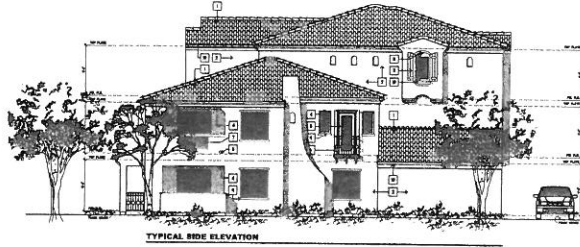
Job No. XXX

Revision

Drawn:

Check:

Sheet



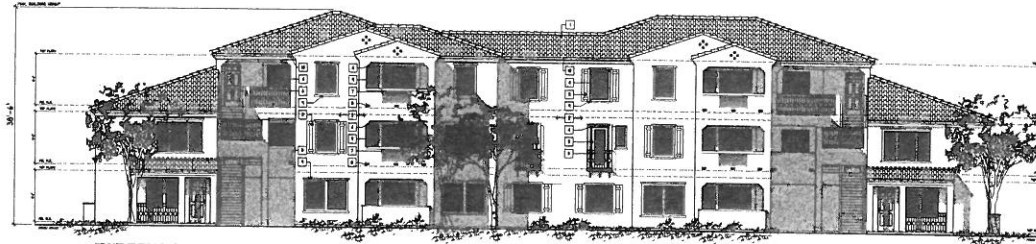
TYPICAL SIDE ELEVATION

TYPICAL ELEVATION NOTES :

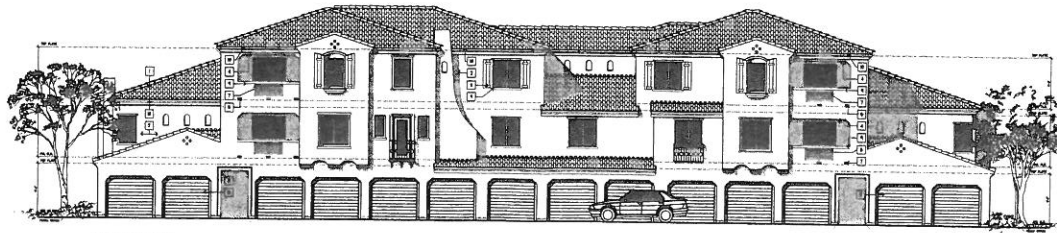
1. CONCRETE W/ TILE ROOFING
2. TYPICAL STUCCO
3. RECESSED WINDOW / DOOR
4. STUCCO SOFFIT
5. STUCCO ARCH
6. W/ RAILING
7. STAGED PIMSON TILES OPEN THRU
8. DECK RAMPERS
9. 2X TRIM WITH STUCCO OVER
10. BUILT UP TRIM WITH STUCCO OVER
11. PAINT SHUTTER
12. GARAGE DOOR

COLOR ELEVATION DESCRIPTION :

- STUCCO - LIGHT CREAM - WHITE
- TILE - MISION W/ TILE TERRAZZOTA BLIND
- SHUTTERS & DOOR - ACCENT COLORS
- VERT. FIN. BLD'G
- HORIZONTAL FIN. BLACK



FRONT ELEVATION



REAR ELEVATION

MR. PAUL REIM  
 OAK PARC PARTNERS, L.P.  
 4101 BIRCH ST., STE. 150  
 NEWPORT BEACH, CA 92669

CONCEPT EXTERIOR ELEVATIONS



Architecture

Bill E. Smith, AIA, Licensed Architect  
 License No. 1211  
 Anaheim, CA 92809  
 (949) 823-3338

Job No. XXX

Revisions:

\_\_\_\_\_

Drawn:

Date:

Sheet

A3

Of



PAGE BREAK



**AIRPORT LAND USE COMMISSION  
RIVERSIDE COUNTY**



March 9, 2015

**CHAIR**

**Simon Housman**  
Rancho Mirage

Mr. David Newell, Associate Planner  
City of Palm Springs Department of Planning Services  
3200 E. Tahquitz Canyon Way  
Palm Springs, CA 92262

**VICE CHAIRMAN**

**Rod Ballance**  
Riverside

**COMMISSIONERS**

**Arthur Butler**  
Riverside

**John Lyon**  
Riverside

**Glen Holmes**  
Hemet

**Greg Pettis**  
Cathedral City

**Steve Manos**  
Lake Elsinore

**STAFF**

**Director**  
Ed Cooper

John Guerin  
Russell Brady  
Barbara Santos

County Administrative Center  
4080 Lerron St., 14<sup>th</sup> Floor  
Riverside, CA 92501  
(951) 955-5132

[www.rcaluc.org](http://www.rcaluc.org)

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW**

File No.: ZAP1026PS15  
Related File No.: Building Permit 2014-3768  
APN: 507-380-026, 507-380-027

Dear Mr. Newell:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC), staff reviewed the above-referenced revised proposal to develop a fitness center associated with the existing stroke recovery center located on the 4.74 acres located northerly of Alejo Road, easterly of Commercial Road, in the City of Palm Springs. The amended project proposes to increase the building height of the previously approved fitness center (previously reviewed by this office as ZAP1016PS13 and found consistent) by 1.55 feet to a maximum of 13.05 feet tall. No changes to the floor plan or the anticipated occupancy of the fitness center are proposed. Although other future project components were noted on the original site plan, the City's current permit and ALUC's review is solely for the current proposed fitness center. Other future project components as shown on the original site plan would require ALUC review once formal application for these structures is submitted to the City. Specifically, development within the portion of the property located within Compatibility Zone B2 would be subject to review by the Commission.

The portion of the property including the proposed specific site of the fitness center is located in Airport Compatibility Zone E of the 2005 Palm Springs Airport Land Use Compatibility Plan. The existing and proposed maximum grade is approximately 447 feet above mean sea level (AMSL), with the proposed building at a revised height of 13.05 feet for a maximum elevation of 460.05 feet AMSL. Based on the distance from the parcel property line to the nearest point of the runway (1,050 feet) and the elevation of the runway at the displaced threshold (448.6 feet AMSL), FAA Obstruction Evaluation review would be required for any structure at the property line exceeding 459.1 feet AMSL. Submittal to FAA Obstruction Evaluation was made and issued an Aeronautical Study Number (ASN) of 2015-AWP-1168-OE, and a Determination of No Hazard to Air Navigation was issued on February 23, 2015.

As ALUC Director, I hereby find the above-referenced revised project **CONSISTENT** with the 2005 Palm Springs Airport Land Use Compatibility Plan, subject to the following conditions:

**CONDITIONS:**

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, artificial marshes, recycling centers containing putrescible wastes, and construction and demolition debris facilities.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached "Notice of Airport in Vicinity" shall be provided to all potential tenants and purchasers.
4. Any new retention basins on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the retention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
5. The Federal Aviation Administration has conducted an aeronautical study of a structure with a height of 14 feet and a maximum elevation at top point of 461 feet above mean sea level (Aeronautical Study Nos. 2015-AWP-1168-OE) and has determined that neither marking nor lighting of such a structure is necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 K Change 2 and shall be maintained in accordance therewith for the life of the project.
6. The maximum elevation of any proposed structure at top point, including all roof-mounted appurtenances (if any), shall not exceed 461 feet above mean sea level.

7. The specific coordinates, height, and top point elevation of proposed structures shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in building height or elevation shall not require further review by the Airport Land Use Commission.
8. Temporary construction equipment used during actual construction of the structures shall not exceed a top point elevation of 461 feet above mean sea level, unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.
9. Within five (5) days after construction of structures reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned.

If you have any questions, please contact Russell Brady, Contract Planner, at (951) 955-0549, or John Guerin, Principal Planner, at (951) 955-0982.

Sincerely,

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Edward C. Cooper, Director

RB:bks

Attachment: Notice of Airport in Vicinity

cc: Stroke Recovery Center – Attn.: Beverly Greer  
Sean Lockyer, AR+D – Attn.: Luis Balderas  
Thomas Nolan, Executive Director, Palm Springs International Airport  
ALUC Case File

Y:\AIRPORT CASE FILES\Palm Springs\ZAP1026PS15\ZAP1026PS15.LTR.doc

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Mail Processing Center  
 Federal Aviation Administration  
 Southwest Regional Office  
 Obstruction Evaluation Group  
 2601 Meacham Boulevard  
 Fort Worth, TX 76193

Aeronautical Study No.  
 2015-AWP-1168-OE

Issued Date: 02/23/2015

Luis Balderas  
 Studio AR&D Architects  
 457 N. Palm Canyon Drive  
 Suite: B  
 Palm Springs, CA 92262

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Building The Fitness Center
Location:	Palm Springs, CA
Latitude:	33-49-52.75N NAD 83
Longitude:	116-30-55.59W
Heights:	447 feet site elevation (SE)
	14 feet above ground level (AGL)
	461 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed and maintained in accordance with FAA Advisory circular 70/7460-1 K Change 2.

This determination expires on 08/23/2016 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates , heights, frequency(ies) and power . Any changes in coordinates , heights, and frequencies or use of greater power will void this determination. Any future construction or alteration , including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

If we can be of further assistance, please contact our office at (425) 227-2625. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2015-AWP-1168-OE.

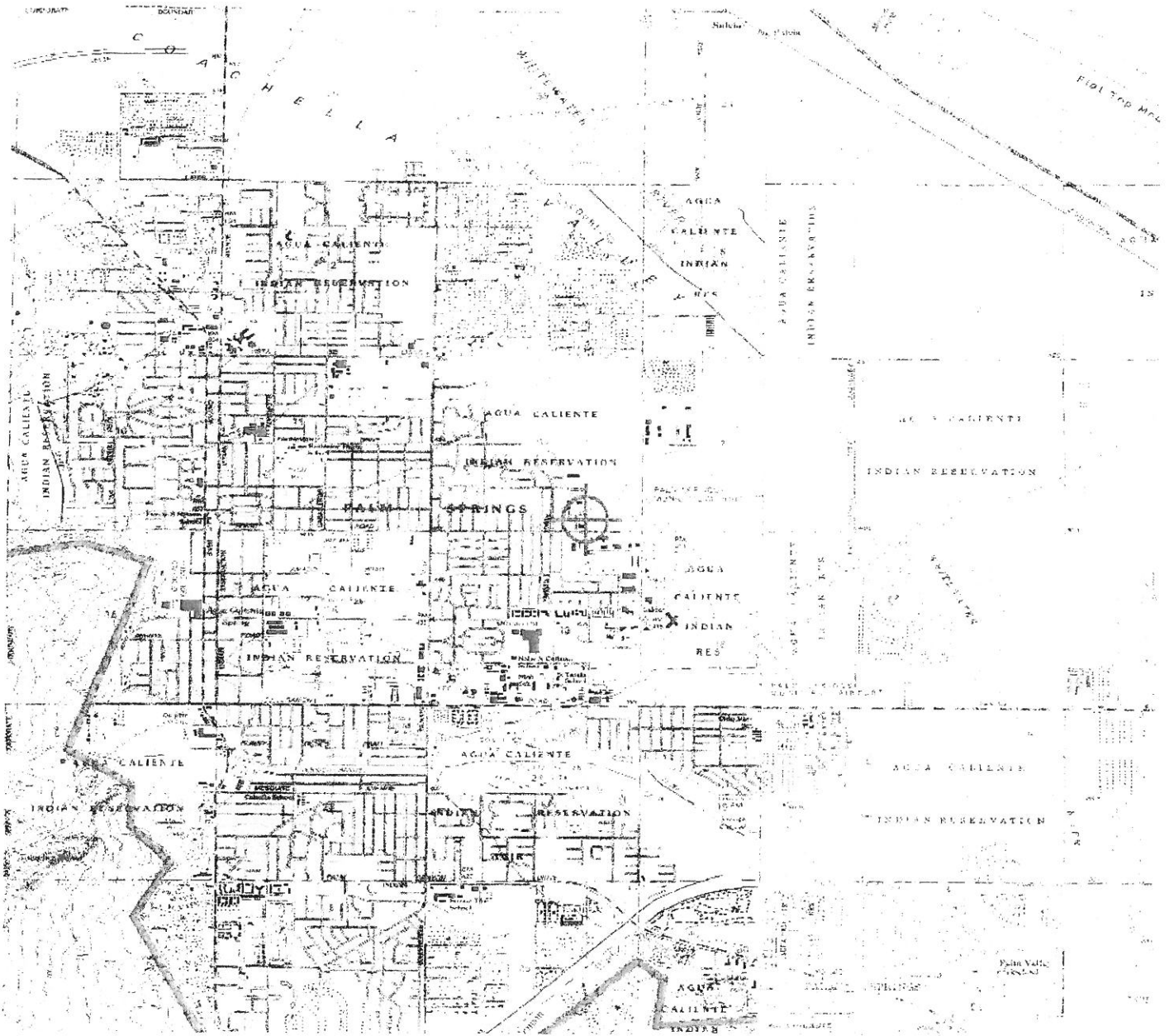
**Signature Control No: 241998721-243905562**

( DNE )

Paul Holmquist  
Technician

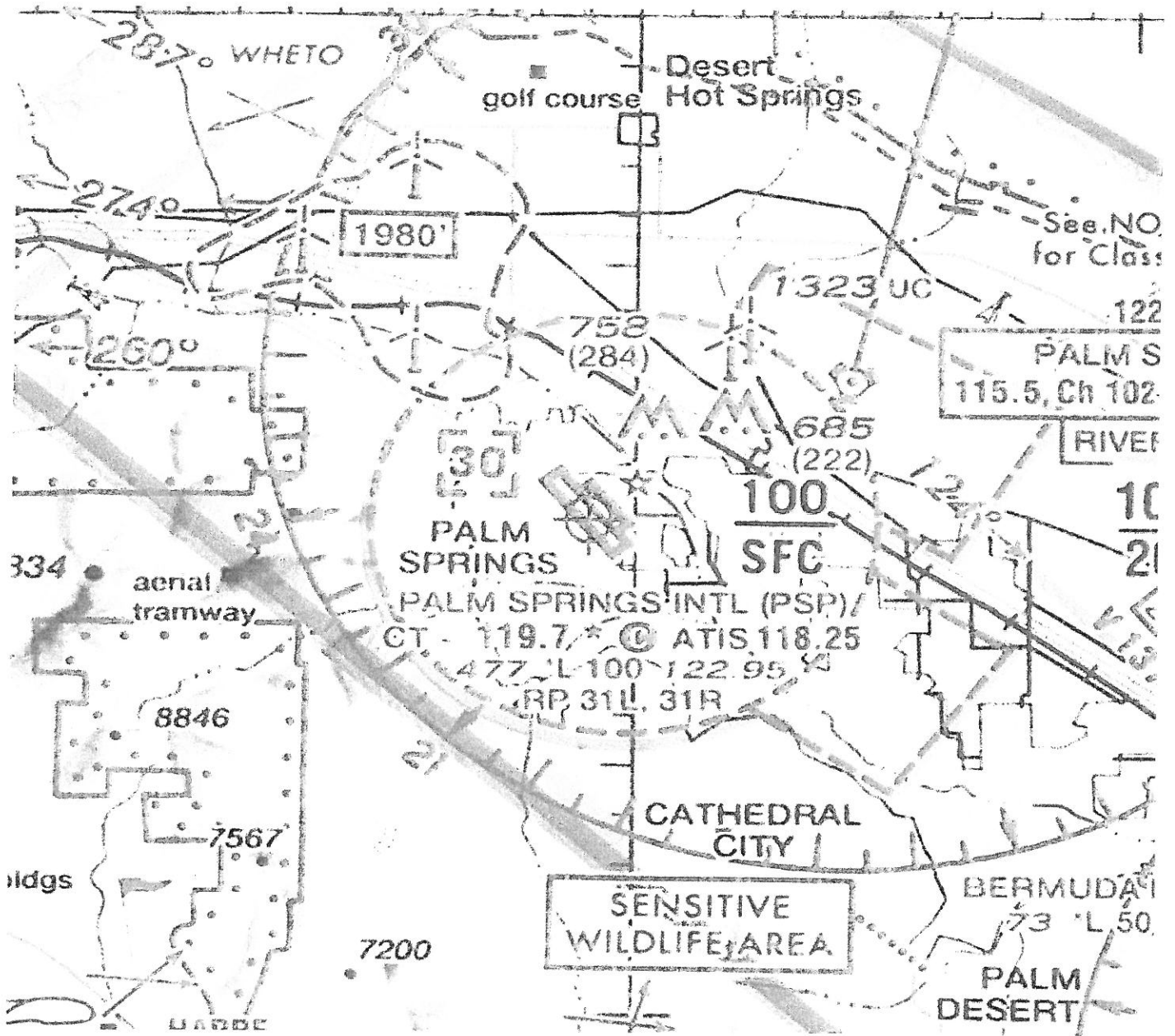
Attachment(s)  
Map(s)

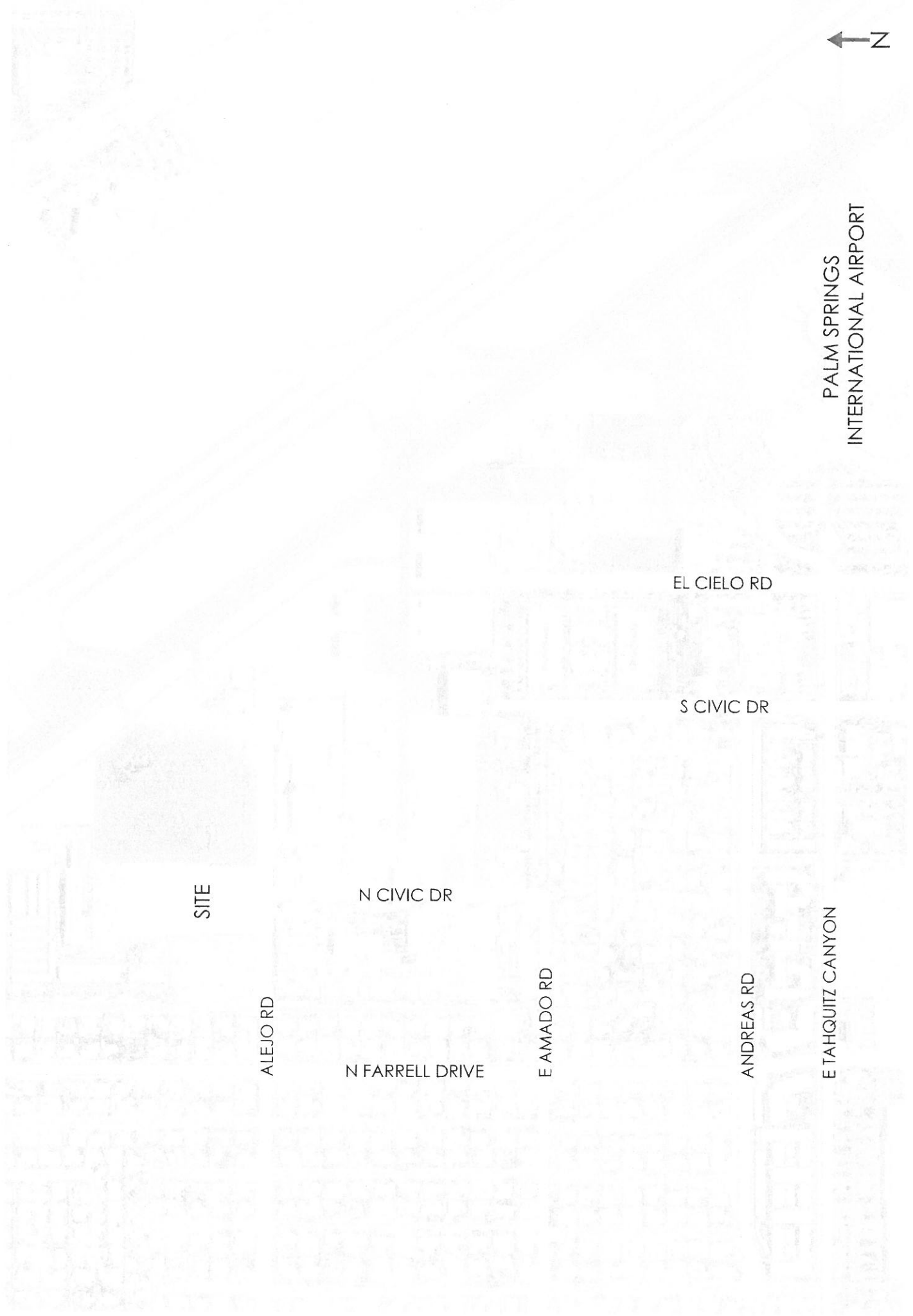
TOPO Map for ASN 2015-AWP-1168-OE





Sectional Map for ASN 2015-AWP-1168-OE





SITE

ALEJO RD

N FARRELL DRIVE

N CIVIC DR

E AMADO RD

ANDREAS RD

EL CIELO RD

S CIVIC DR

E TAHQUITZ CANYON

PALM SPRINGS  
INTERNATIONAL AIRPORT



VICINITY MAP

NOTES: WALKWAYS ALONG ACCESSIBLE ROUTE OF TRAVEL ARE REQUIRED TO BE 48" MINIMUM WITH HARDWARE SURFACE-TANT SURFACES PER SECTION 5102.

THE MAXIMUM UNCOMPLETED CROSS SLOPE SHALL BE 1/4" PER FOOT PER SECTION 5102(1).

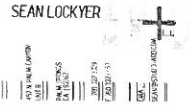
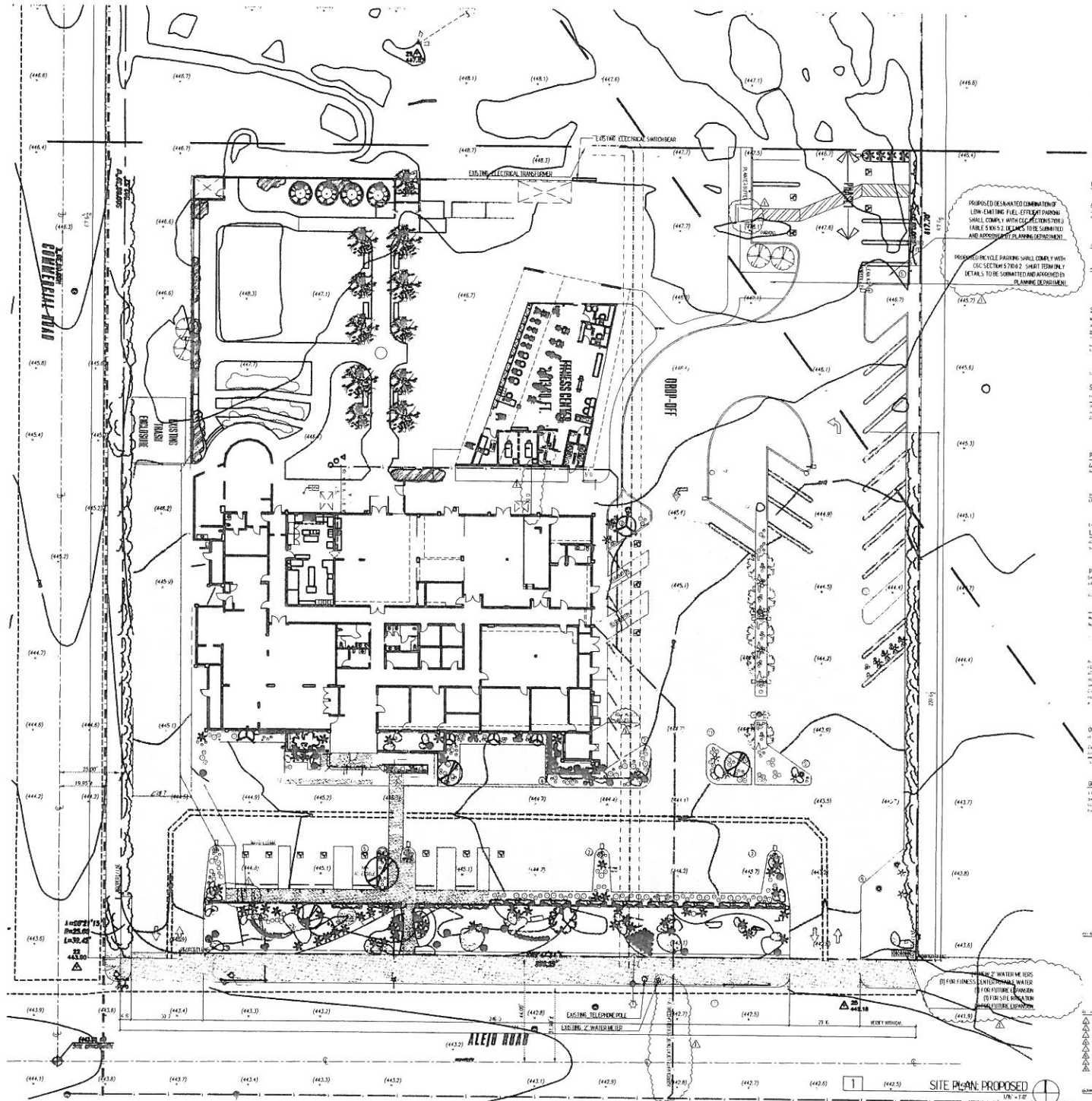
ANY ABOVE LEVEL CHANGES SHALL BE 1/2" ALONG ANY ACCESSIBLE ROUTE OF TRAVEL. WHEN CHANGES OCCUR, THEY SHALL BE REVEALED WITH A SLOPE OF 1:2. LEVEL CHANGES OF 1/4" MAY BE VERTICAL.

1/2" LEVEL CHANGES > 1/2": THEY MUST COMPLY WITH THE REQUIREMENTS OF 5106(4)(B).

FROM ALL WALKWAY SURFACES TO OBSTRUCTIONS A 30" HEADROOM IS TO BE PROVIDED PER SECTION 5102(2).

VEHICLE PARKING SHALL COMPLY WITH SSC SECTION 5103(2).

VEHICLE EFFICIENT VEHICLE PARKING WILL BE PROVIDED IN ACCORDANCE WITH SSC SECTION 5103(3).



**PROJECT DATA**

Project Name	THE NEW FITNESS CENTER
Client	THE STROKE RECOVERY CENTER
Address	2800 E. ALEJO ROAD, PALM SPRINGS, CA 92262
Site Area	1.5 ACRES
Project No.	2018-001
Scale	1/8" = 1'-0"
Date	10/15/2018

**GROUND FOOTAGE (EXISTING)**

Area	15,000 sq. ft.
Perimeter	1,200 ft.

**GROUND FOOTAGE (PROPOSED)**

Area	18,000 sq. ft.
Perimeter	1,500 ft.

**PALM SPRINGS ZONING DATA**

Zoning District	SP-1
Maximum Building Height	35 FT.
Maximum Floor Area Ratio	0.5
Maximum Lot Coverage	25%

**LANDSCAPE ZONE DATA**

Zone	Zone 1
Planting Density	100 plants per acre
Water Feature	Required

**PARKING REQUIREMENTS (PHASE 1 ONLY)**

Vehicle Type	Standard	150
Vehicle Type	Handicap	10
Vehicle Type	Motorcycle	5

**ODD-PARCELS LINES**

Parcel No.	123456	Area	0.5 AC
Parcel No.	123457	Area	0.5 AC

**GENERAL NOTES**

1. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE PALM SPRINGS ZONING ORDINANCE AND THE CALIFORNIA BUILDING CODE.
2. THE DESIGNER SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE PALM SPRINGS CITY ENGINEER AND PLANNING DEPARTMENT.
3. THE DESIGNER SHALL PROVIDE A DETAILED LANDSCAPE PLAN AND WATER FEATURE PLAN AS PART OF THE SUBMITTAL.
4. THE DESIGNER SHALL PROVIDE A DETAILED UTILITY PLAN AS PART OF THE SUBMITTAL.

**THE NEW FITNESS CENTER**  
**THE STROKE RECOVERY CENTER**  
 2800 E. ALEJO ROAD  
 PALM SPRINGS, CA 92262  
 APR 507-866-0202  
 507-866-0207



WATER MEISS  
 (1) FOR FITNESS CENTER  
 (2) FOR FUTURE PARKING  
 (3) FOR USE IN FUTURE DEVELOPMENT

EXISTING TELEPHONE  
 EXISTING WATER MAIN

STREET LIGHTING

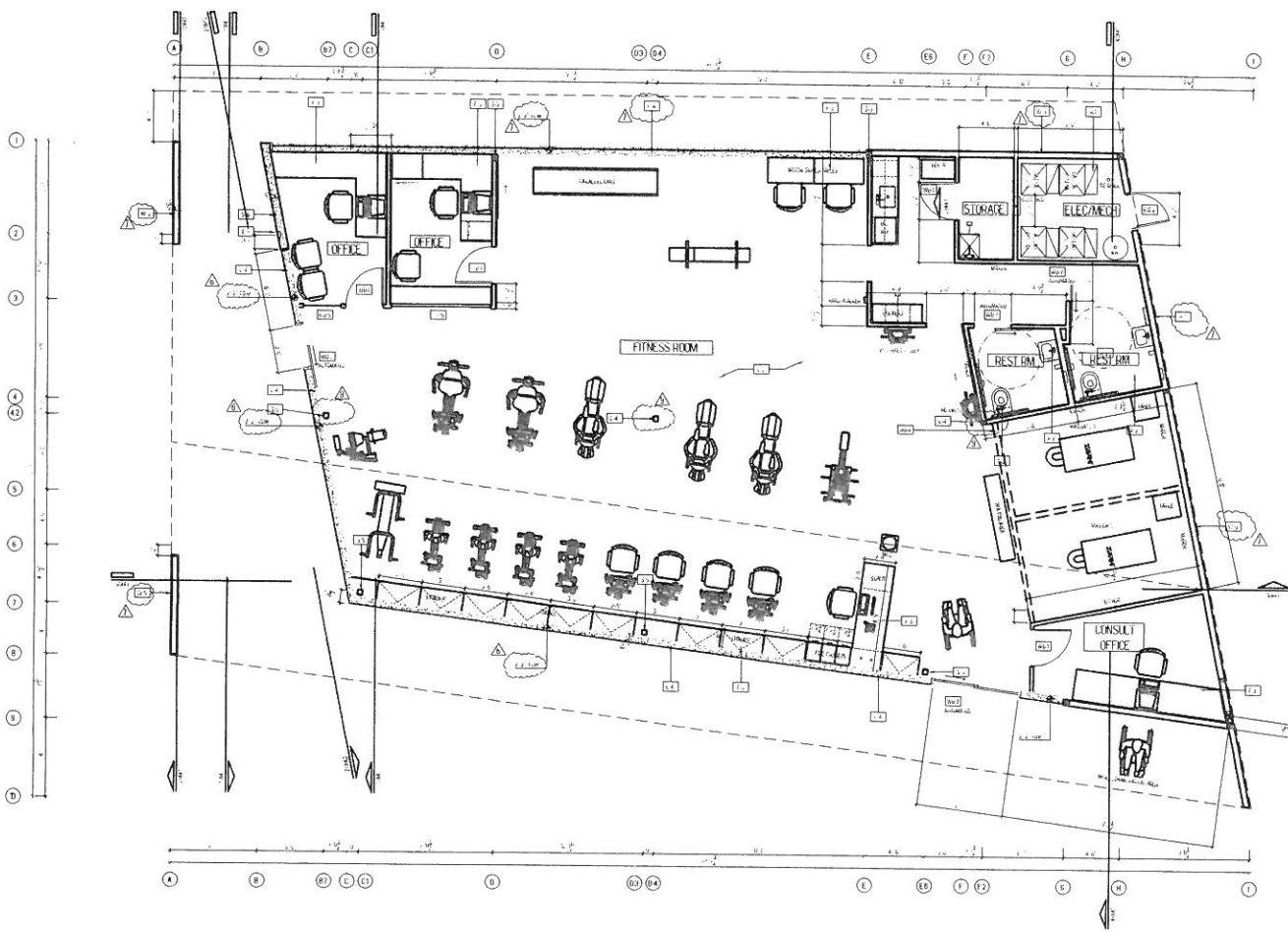
1/8" = 1'-0"

SITE PLAN - PROPOSED



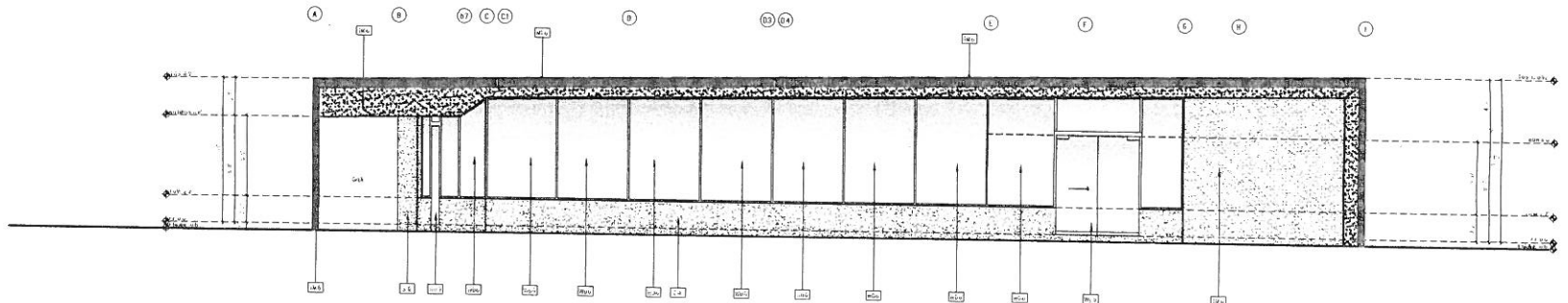
**LEGEND**

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FITNESS CENTER PLAN: PROPOSED

THE NEW FITNESS CENTER  
 THE STRIKE RECOVERY CENTER  
 2610 E ALEXANDER  
 PALM SPRINGS, CA 92262  
 APN 507-380-056  
 951-880-027

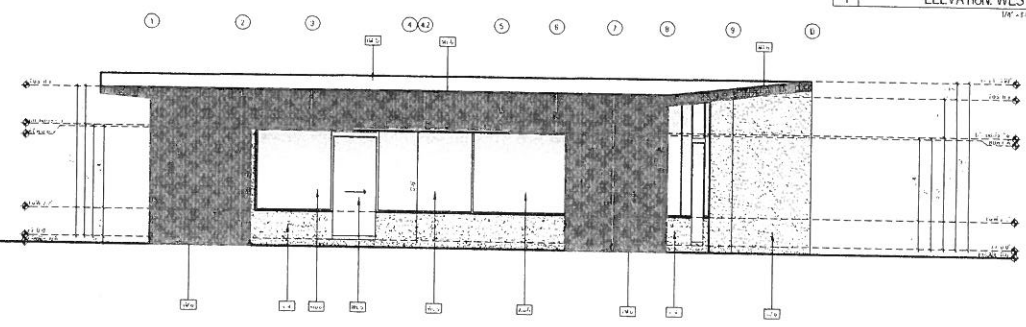


SEAN LOCKYER  
 ARCHITECT  
 1000 S. PALM BLVD. SUITE 100  
 PALM BEACH, FL 33480  
 (561) 833-1111  
 www.seanlockyer.com

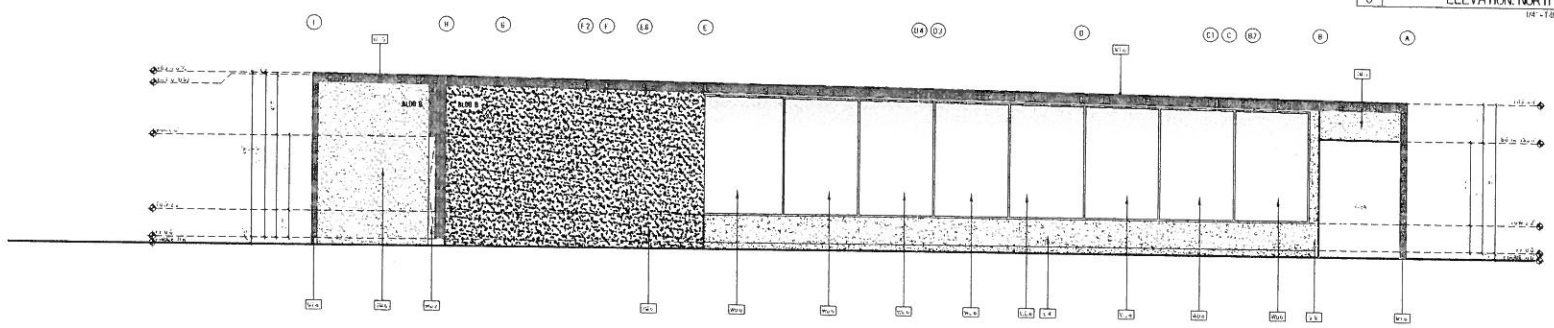
**LEGEND**

1	CONCRETE
2	INSULATION
3	BRICK
4	GLASS
5	WOOD
6	STEEL
7	ROOFING
8	LANDSCAPE
9	MECHANICAL
10	ELECTRICAL
11	PLUMBING
12	PAINT
13	FINISH
14	FOUNDATION
15	STRUCTURE
16	MECHANICAL
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93	MECHANICAL
94	ELECTRICAL
95	PLUMBING
96	PAINT
97	FINISH
98	FOUNDATION
99	STRUCTURE
100	MECHANICAL

4 ELEVATION: WEST  
 10'-0" = 1"



3 ELEVATION: NORTH  
 10'-0" = 1"



2 ELEVATION: EAST  
 10'-0" = 1"

SEE ALSO FOR WINDOW SIZES AND MATERIALS  
 SEE ARCHITECTURAL SPECIFICATIONS

**KEY:**

1	CONCRETE
2	INSULATION
3	BRICK
4	GLASS
5	WOOD
6	STEEL
7	ROOFING
8	LANDSCAPE
9	MECHANICAL
10	ELECTRICAL
11	PLUMBING
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93	MECHANICAL
94	ELECTRICAL
95	PLUMBING
96	PAINT
97	FINISH
98	FOUNDATION
99	STRUCTURE
100	MECHANICAL

1 ELEVATION: SOUTH  
 10'-0" = 1"

**LEGEND**

1	CONCRETE
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 THE STROKE RECOVERY CENTER  
 DRP E. ALEXANDER  
 PALM BEACH, FL 33480  
 561-833-1111  
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