

AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY AGENDA

Riverside County Administration Center
4080 Lemon Street, 1st Floor Hearing Room
Riverside, California

Thursday 9:00 A.M., September 10, 2015

CHAIR
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Rancho Mirage

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County Administrative Center
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NOTE: If you wish to speak, please complete a "SPEAKER IDENTIFICATION FORM" and give it to the Secretary. The purpose of the public hearing is to allow interested parties to express their concerns. Comments shall be limited to 5 minutes and to matters relevant to the item under consideration. Please do not repeat information already given. If you have no additional information, but wish to be on record, simply give your name and address and state that you agree with the previous speaker(s). Also please be aware that the indicated staff recommendation shown below may differ from that presented to the Commission during the public hearing.

Non-exempt materials related to an item on this agenda submitted to the Airport Land Use Commission or its staff after distribution of the agenda packet are available for public inspection in the Airport Land Use Commission's office located at 4080 Lemon Street, 14th Floor, Riverside, CA 92501 during normal business hours.

In compliance with the Americans with Disabilities Act, if any accommodations are needed, please contact Barbara Santos at (951) 955-5132 or E-mail at basantos@rctlma.org. Request should be made at least 48 hours or as soon as possible prior to the scheduled meeting.

1.0 INTRODUCTIONS

1.1 CALL TO ORDER

1.2 SALUTE TO FLAG

1.3 ROLL CALL

2.0 PUBLIC HEARING: CONTINUED ITEM

CHINO AIRPORT

- 2.1 ZAP1021CH15 – Moons Family Trust (Representative: Summit Development Corporation, Bryan Bentrott and Taylor Gerry) – City of Eastvale Case No. 15-0783 (Specific Plan Amendment [SPA], Tentative Parcel Map [TPM 36787], and Major Development Plan [MDP]. The SPA proposes an amendment to the Ranch at Eastvale Specific Plan located southerly of the San Bernardino County line, easterly of Hellman Avenue, and westerly of Cucamonga Creek Channel in the City of Eastvale to change the land use designation of Planning Area 2 from Commercial/Retail to Mixed Use, modify the boundaries between, and acreages of, the Planning Areas, and modify the permitted uses within each Planning Area. Limonite Avenue will ultimately be extended through this site to connect to Kimball Avenue in the City of Chino. The TPM proposes to subdivide 82.16 net acres (97.98 gross acres) into 15 parcels. The MDP proposes to develop a total of 940,000 square feet of industrial space within 6 shell buildings on 50.09 net acres (56.03 gross acres) (Lots 1-7 of the TPM). Size of each building ranges between 50,000 square feet to 300,000 square feet with anticipated uses primarily consisting of warehousing with supporting offices. **FURTHER CONSIDERATION: This project is being reconsidered due to changes in locations of required open areas.** (Compatibility Zones B1 and C of Chino Airport Influence Area). ALUC Staff Planner: John Guerin at (951) 955-0982, or e-mail at jguerin@rctlma.org

Recommendation: CONSISTENT

3.0 PUBLIC HEARING: NEW ITEMS**MARCH AIR RESERVE BASE**

- 3.1 ZAP1138MA15 – R.C. Jenson, Inc., dba Jenson USA (Representative: Jim Broeske, Broeske Architects & Associates, Inc.) - City of Riverside Building Department. The applicant proposes to add 2,500 square feet of first floor office area and 2,730 square feet of second floor office area within an existing industrial warehouse building with an address of 1615 Eastridge Avenue, located at the southeasterly corner of Eastridge Avenue and Lance Drive. These improvements would increase the net building area to 73,652 square feet. (Airport Compatibility Zone C1 of the March Air Reserve Base/Inland Port Airport Influence Area). ALUC Staff Planner: John Guerin at (951) 955-0982, or e-mail at jguerin@rctlma.org

Staff Recommendation: CONSISTENT

- 3.2 ZAP1135MA15 – Jack Kofdarali and Ino Cruz/French Valley Benton Rd., LP – City of Moreno Valley Case No.: PA15-0032 (Conditional Use Permit). The applicant proposes to develop and operate a 48,833 square foot industrial warehouse (including 5,446 square feet of office space), a gasoline and diesel service station with 14 pumps, a 3,800 square foot convenience store, three fast-food restaurants with a combined gross floor area of 9,200 square feet, and a car wash on 6.31 acres located northerly of Cactus Avenue, easterly of Commerce Center Drive, and southerly of Goldencrest Drive in the City of Moreno Valley. (Assessor's Parcel Numbers 297-130-052, -053, and -054) (Airport Compatibility Zone B2 of the March Air Reserve Base/Inland Port Airport Influence Area). ALUC Staff Planner: John Guerin at (951) 955-0982, or e-mail at jguerin@rctlma.org

Staff Recommendation: CONTINUE to 10-8-15

JACQUELINE COCHRAN REGIONAL AIRPORT

- 3.3 ZAP1033TH15 – Ross & Sons Refrigeration & Construction, for Anthony Vineyards, Inc. (Representative: Audrey Nickerson, The Altum Group) – City of Coachella Architectural Review Case No. 15-05. The landowner proposes to construct a new 33,560 square foot freezer (cold storage) building and a 14,816 square foot open canopy to supplement the existing cold storage building and canopy at the Anthony Vineyards facility located on 8.48 acres at the northwest corner of Enterprise Way and Industrial Way. (Airport Compatibility Zone C of the Jacqueline Cochran Regional Airport Influence Area). ALUC Staff Planner: John Guerin at (951) 955-0982, or e-mail at jguerin@rctlma.org

Staff Recommendation: CONSISTENT

- 3.4 ZAP1032TH15 – Fullerton Architects PC (Nick Fullerton), for Allen Russak – County Case No. BRS 150586 (Building Permit). The applicant proposes to install and operate roof mounted photovoltaic solar panels on a private member's seasonal residence/garage located at 61776 Fullerton Drive (Lot 202 of Thermal Club – Assessor's Parcel Number 759-220-028) within the motorsports facility located northerly of 62nd Avenue, easterly of Tyler Street, westerly of Polk Street, and southerly of 60th Avenue in the unincorporated community of Thermal (Zone D of the Jacqueline Cochran Regional Airport Influence Area). ALUC Staff Planner: John Guerin at (951) 955-0982, or e-mail at jguerin@rctlma.org

Staff Recommendation: CONSISTENT

BERMUDA DUNES AIRPORT

- 3.5 ZAP1059BD15 – Wagner Architecture Group, Inc., for GEOVEL, Inc. (Representative: Jessica Peat or Walt Wagner) – City of Indio Case Nos. CUP 15-6-009 (Conditional Use Permit) and DR 15-5-387 (Design Review). A proposal to establish an automobile dealership (Chrysler Dodge Jeep Ram) within a 4-acre portion of a 17.94-acre parcel located on the northerly side of Varner Road, easterly of its intersection with Adams Street. (The parcel - Assessor's Parcel Number 607-230-002 – extends northerly to 40th Avenue.) The applicant proposes a 41,413 square foot main building (36,921 square feet excluding canopy) and a 1,300 square foot car wash. An additional 12,115 square foot building and outdoor parking spaces are proposed on an adjacent 4.95 acres of the same parcel. (Airport Compatibility Zones B1, B2 and D of the Bermuda Dunes Airport Influence Area). ALUC Staff Planner: John Guerin at (951) 955-0982, or e-mail at jguerin@rctlma.org

Staff Recommendation: INCONSISTENT

4.0 ADMINISTRATIVE ITEMS**4.1 Director's Approvals****4.2 Video/Live Streaming of Meetings Held in the County Administrative Center Board Room****5.0 ORAL COMMUNICATION ON ANY MATTER NOT ON THE AGENDA****6.0 COMMISSIONER'S COMMENTS**

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 2.1 ~~3-7~~

HEARING DATE: September 10, 2015 (previously heard on July 9, 2015)

CASE NUMBER: ZAP1021CH15 – Moons Family Trust (Representative: Summit Development Corporation, Bryan Bentrout and Taylor Gerry)

APPROVING JURISDICTION: City of Eastvale

JURISDICTION CASE NO.: 15-0783 (Specific Plan Amendment, Tentative Parcel Map [TPM 36787], and Major Development Plan)

MAJOR ISSUES: Proposed Buildings 3 and 4 are calculated to have a single-acre intensity of 87, which does not comply with the standard Zone B1 single-acre criterion of 80. However, the project includes four out of the seven risk reduction measures for an adjusted single-acre criterion of 92 with a 15% bonus. At this adjusted single-acre criterion, the calculated single-acre intensities of 87 would comply.

In the past, ALUC staff has allowed land within a project's internal roadways and half-widths of external roadways to be credited toward open area minimum requirements, provided that such areas, either independently or in combination with adjacent areas, met the minimum dimension requirements (75 feet in width and 300 feet in length) for unobstructed open areas. However, the City of Eastvale has recently expressed concerns related to designation of open area within its public rights-of-way and has advised that no open area should be designated within Limonite Avenue or any other publicly dedicated roadways. The applicant had prepared exhibits on the assumption that open area within the roadways would be acceptable, and is in the process of updating exhibits for this project to meet ALUC's open area requirements without use of areas within the public rights-of-way.

The applicant did prepare exhibits meeting ALUC's open area requirements without use of areas within public rights-of-way in time for the Commission to issue a finding of conditional consistency at its July 9 public hearing. However, following the Commission hearing, the applicant team submitted additional open area exhibits that staff had (incorrectly, as it turns out) interpreted to be post-hearing revisions to satisfy the lighting requirements of the City of Eastvale. Consequently, staff required that this project return to the Commission for a review of this most recent open area exhibit, and to provide for a change in ALUC's recommended conditions to reflect the date of the most recent open area exhibit. The most recent open area exhibit is included as an attachment directly behind this staff report.

In the meantime, FAA Obstruction Evaluation Service determinations have been issued for each of the six proposed buildings.

RECOMMENDATION:

Staff recommends that the project, as amended to include the open area exhibit dated August 24, 2015, be found CONSISTENT with the 2008 Chino Airport Land Use Compatibility Plan, subject to the conditions included herein.

~~Staff recommends that the project be CONTINUED to August 13, 2015 pending revision of the project's open area exhibits and confirmation that the City of Eastvale finds ALUC staff's recommendation for the risk reduction intensity bonus acceptable. However, if an updated exhibit is provided prior to the hearing that meets our open area requirements without use of public rights-of-way, then, provided that the City of Eastvale finds ALUC staff's recommendation for the risk reduction intensity bonus acceptable, staff would recommend that the project be found CONDITIONALLY CONSISTENT, subject to the conditions included herein and such additional conditions as may be required by the Federal Aviation Administration (FAA) Obstruction Evaluation Service.~~

PROJECT DESCRIPTION: The Specific Plan Amendment proposes to change the land use designation of Planning Area 2 from Commercial/Retail to Mixed Use, modify the boundaries between, and the acreages of, the Planning Areas, and modify the permitted uses within each Planning Area of The Ranch at Eastvale Specific Plan. The Tentative Parcel Map proposes to subdivide 82.16 net acres (97.98 gross acres) into 15 parcels. The Master Development Plan proposes to develop a total of ~~945,000~~ **940,000** square feet of industrial space within ~~7~~ **six (6)** shell buildings on 50.09 net acres (56.03 gross acres) (lots 1-7 of the Tentative Parcel Map). The proposed buildings range in size from 50,000 to 300,000 square feet, with anticipated uses primarily consisting of warehousing, with supporting offices.

PROJECT LOCATION: The site is located southerly of the San Bernardino County line, easterly of Hellman Avenue, and westerly of Cucamonga Creek Channel, in the City of Eastvale, approximately 3,900 feet easterly of the easterly terminus of Runway 8R-26L at Chino Airport. Limonite Avenue will ultimately be extended through this site to connect to Kimball Avenue in the City of Chino.

LAND USE PLAN: 2008 Chino Airport Land Use Compatibility Plan

- a. Airport Influence Area: Chino Airport
- b. Land Use Policy: Airport Compatibility Zones B1 and C
- c. Noise Levels: Primarily beyond the 55 CNEL contour; 55-60 CNEL in the northeasterly portion of the property

ANALYSIS:

Nonresidential Average Intensity: The site is located in Airport Compatibility Zones B1 and C, with the majority of the buildings located entirely or mostly within Zone C. Buildings 1, 5, and 6, and 7 are located entirely within Zone C. Building 2 is located mostly within Zone C. Buildings 3 and 4 are located mostly within Zone B1. As shown on the site plan and floor plans, no office area is proposed within Zone B1.

Nonresidential intensity in Airport Zones B1 and C is restricted to averages of 40 and 75 persons per acre, respectively. The “Building Code Method” for calculating intensity utilizes “minimum floor area per occupant” criteria from the Building Code as a factor in projecting intensity. Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, the intensity of office areas has been determined to be one person per 100 square feet. However, Appendix C recommends that, for calculation of intensity levels, the Building Code occupancy levels be reduced by 50 percent, at least for office uses. Warehousing uses are calculated at one person per 500 square feet.

Based on these building intensity rates, the ~~six~~ 7 buildings are anticipated to accommodate a total of 2,026 people based on the anticipated distribution of office and warehouse uses indicated on the site plan submitted. Divided between the two zones and the uses within each zone, Zone B1 would accommodate a total of 755 people and Zone C would accommodate 1,271 people. To determine the average intensity for each zone, we divide by the gross acreage of each zone, which is 20.14 acres for Zone B1 and 35.89 acres for Zone C. At these gross acreages, an average intensity of 37 people is estimated for Zone B1 and an average intensity of 35 is estimated for Zone C, which are both consistent with the respective Zone B1 and C average acre intensity criteria.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per standard vehicle and 1.0 persons per truck trailer space in the absence of more precise data). Based on the number of parking spaces provided (1,050 standard vehicle spaces and 240 loading/trailer spaces), the total occupancy would be estimated at 1,815 people for an average acre intensity of approximately 32 across the entire site, which is also consistent with both the Zone B1 and C average acre intensity criteria. Looking specifically at those buildings located partially within Zone B1 (Buildings 2, 3, and 4) and the amount of parking spaces for each building (regardless of the Zone the parking spaces are located in) the average amount of people based on the proposed parcel size ranges between 25 and 36 persons, which is also consistent with the Zone B1 average acre intensity criteria.

Development is only currently proposed on parcels 1-7 of the Tentative Parcel Map within Planning Areas 1 and 4 of the Specific Plan. The intensity of parcels 8-15 and Planning Areas 2-3 and 5-9 would be determined when specific development is proposed on those parcels. While certain uses permitted by the Specific Plan (i.e. hotels/motels, restaurants, churches, theaters, etc.) may possibly

exceed the intensity criteria of their respective Compatibility Zone, this cannot be determined absolutely at this time without a specific proposal to calculate the actual intensity. Although such uses may be permitted by the Specific Plan, they would still be subject to further ALUC review to determine whether the specific proposal is consistent with the applicable intensity criteria. The permitted uses table in the proposed Specific Plan specifically labels typically high intensity uses that should consider the particular zone location and size of their facility relative to the applicable zone compatibility criteria.

Non-Residential Single-Acre Intensity: As previously noted, the project is located in Airport Compatibility Zones B1 and C. Non-residential intensity in Airport Compatibility Zones B1 and C is restricted to 80 and 150 persons, respectively, in any given single-acre pursuant to the Chino Airport Land Use Compatibility Plan. Since end uses and floor plans are not specifically known, analysis of anticipated uses assumes a worst case scenario of the maximum amount of office, then the maximum amount of warehouse that could fit within any given single-acre area (210 foot by 210 foot area) utilizing the anticipated area for each use in each building as indicated by the site plan provided. Such a worst case scenario single-acre area would include some non-building areas based on the single-acre boundaries. The single-acre boundaries are only located within one zone (do not overlap zones) to accurately reflect the intensity within a given zone to compare to the applicable zone criteria.

Based on this methodology and the Building Code method (with 50% reduction for office uses), following are the calculations of the most intense single-acre for each zone and building. This indicates that the single-acre criterion of 80 for Zone B1 is exceeded for Buildings 3 and 4. All other single-acre intensities would be consistent with the respective Zone B1 and C criteria.

Zone B1

- Building 2 – warehouse 9,576 square feet – 19 people
- Building 3 – warehouse 43,560 square feet – 87 people
- Building 4 – warehouse 43,560 square feet – 87 people

Zone C

- Building 1 – office 5,000 square feet, warehouse 39,560 square feet – 104 people
- Building 2 – office 5,000 square feet, warehouse 41,060 square feet – 107 people
- Building 3 – office 5,000 square feet, warehouse 28,500 square feet – 82 people
- Building 4 – office 5,000 square feet, warehouse 14,377 square feet – 54 people
- Building 5 – office 10,000 square feet, warehouse 33,335 square feet – 117 people
- Building 6 – office 10,000 square feet, warehouse 34,060 square feet – 118 people
- ~~Building 7 – office 5,000 square feet, warehouse 33,210 square feet – 92 people~~

Risk Reduction Bonus: Pursuant to Countywide Policy 4.2.6, Risk Reduction is not applicable within Zone B1 for runways routinely used by large aircraft (aircraft having a maximum certified takeoff weight of more than 12,500 pounds). “Routinely” is not clearly defined in the policy, but in

discussions with Mead & Hunt who prepared the plan and policies, it was noted that this exclusion was generally intended to apply to March Air Reserve Base/Inland Port and Palm Springs International Airport only since they primarily accommodate large aircraft and not other airports within Riverside County.

Buildings 3 and 4 which exceed the typical Zone B1 single-acre criterion of 80 will include design components that would allow consideration of a risk reduction bonus. If a risk reduction bonus is considered by the City of Eastvale, the previously noted maximums for each use and those noted in the following conditions may be increased to account for the potentially granted risk reduction bonus, which cannot exceed 30 percent.

Buildings 3 and 4 would include concrete walls, limited windows, enhanced fire sprinkler system (Early Suppression Fast Response [ESFR]), and increased emergency exits (15 required, 19 provided for Building 3; 24 required, 30 provided for Building 4). Based on these buildings including at least four out of the recommended seven risk reduction design measures, it is suggested that half of the maximum 30 percent bonus be considered by the City. This would equate to a 15 percent bonus which would increase the standard 80 person intensity criteria to 92 persons. As noted previously, Buildings 3 and 4 would result in a maximum of 87 persons in a single-acre. These would be consistent with a revised intensity criteria with bonus of 92 persons.

Open Area: 30% open area is required within Compatibility Zone B1, and 20% open area is required within Compatibility Zone C. The gross area of the MDP site is 56.03 acres with 20.14 acres within Zone B1 and 35.89 acres within Zone C. Based on these acreages, 6.042 acres are required within Zone B1 and 7.178 acres are required within Zone C for a total of 13.22 acres of open area are required.

As indicated on the submitted Open Space Area exhibit, the proposed project provides ~~6.25~~ 6.24 acres within Zone B1 and ~~7.33~~ 7.18 acres within Zone C for a total of ~~13.58~~ 13.42 acres of open area (areas a minimum of 75'x 300') within certain trailer and vehicle parking areas, and drive aisles, and roadways.

~~However, this calculation regarding open areas provided are based on exhibits that the applicant had prepared on the assumption that crediting of open areas within the roadways would be acceptable (as such crediting has been deemed acceptable by ALUC staff in the past). As noted above, the City of Eastvale has recently expressed concerns related to designation of open area within its public rights-of-way and has advised that no open area should be designated within Limonite Avenue or any other publicly dedicated roadways. The applicant is in the process of updating exhibits for this project to meet ALUC's open area requirements without use of areas within the public rights-of-way.~~

Precise development and open area plans are only available for parcels 1-7 of the Tentative Parcel Map within Planning Areas 1 and 4 of the Specific Plan. Compliance with open area requirements for parcels 8-15 and Planning Areas 2-3 and 5-9 would be determined when specific development is proposed on those parcels.

Prohibited and Discouraged Uses: Churches/places of assembly and day care centers are allowed in certain Planning Areas, but churches are not allowed within the Planning Areas located within Compatibility Zone B1 (Planning Area 1) and day care centers are not allowed within Planning Areas located within Compatibility Zones B1 and C (Planning Areas 1, 2, 3, 4, 6, 7, 8, and 9). Although the proposed Specific Plan Amendment would allow certain typically intense land uses, it does not allow for any uses that are prohibited or discouraged within Compatibility Zones B1 or C. As noted previously, any future development would still be subject to further ALUC review to determine whether the specific proposal is consistent with the applicable intensity criteria.

Projects within Compatibility Zone B1 are required to locate structures a maximum distance from the extended runway centerline. The extended runway centerline does not cross the proposed Master Development Plan area, but would cross within the northern portion of Planning Area 7 of the Specific Plan. The Master Development Plan proposes buildings that would be set back a minimum of approximately 370 feet from the extended runway centerline located to the north. No development is proposed within Planning Area 7 of the Specific Plan at this time, but development therein would need to be reviewed for adequacy of setback from the extended runway centerline when specific development is proposed.

Noise: The northeast portion of the site is located partially within an area that is projected in the 2008 Chino Airport Land Use Compatibility Plan to ultimately be subject to average aircraft noise levels between 55 and 60 CNEL. The remainder of the site falls below the 55 CNEL level. Typical construction design would allow for an exterior to interior noise reduction of at least 20 dbA. Interior noise levels would likely not exceed 40 CNEL for the anticipated industrial and office uses within the proposed buildings. Therefore, no special noise mitigation measures will be required to reduce interior noise levels from aircraft operations.

PART 77: The elevation of Runway 8R-26L at its easterly terminus (the nearest point to the site) is 636.5 feet above mean sea level (AMSL). At an approximate distance of 3,900 feet from the runway, any building with an elevation at top of roof exceeding 675.5 feet AMSL would require Federal Aviation Administration (FAA) Obstruction Evaluation Service notice and review through the Form 7460-1 process. According to the proposed grading plan, the highest pad elevation is 643.4 feet AMSL (building 4). The proposed buildings will not exceed a height of 43 feet above ground level for a total maximum height of 686.4 feet AMSL. Therefore, Obstruction Evaluation is required. Each of the ~~six~~ **seven** buildings has been submitted to the FAA Obstruction Evaluation Service **and has received a Determination of No Hazard to Air Navigation (copies of which are attached to this staff report).** ~~and aeronautical studies are in a Work in Progress status.~~

CONDITIONS:

For the Specific Plan:

1. All entitlement applications within the Specific Plan shall be submitted to Airport Land Use

Commission for review.

For the Master Development Plan:

1. Any outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky.
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light, or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area, including landscaping utilizing water features, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, and incinerators.
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Within Compatibility Zone B1: Children's schools, day care centers, libraries, hospitals, nursing homes, places of worship, buildings with more than 2 aboveground habitable floors, highly noise-sensitive outdoor non-residential uses, aboveground bulk storage of hazardous materials, and hazards to flight.
 - (f) Within Compatibility Zone C: Children's schools, day care centers, libraries, hospitals, nursing homes, buildings with more than 3 aboveground habitable floors, highly noise-sensitive outdoor non-residential uses, and hazards to flight.
3. The City of Eastvale shall either prohibit the following uses, or shall require additional review by the Airport Land Use Commission prior to the establishment of any of the following uses in any of the structures proposed through this Major Development Plan:

Auction rooms, auditoriums, bowling alleys, churches and chapels, classrooms, conference rooms, restaurant serving area (dining areas and areas open to public use, other than corridors

and restrooms), dance floors, drinking establishments, exhibit rooms, gaming, gymnasiums, lodge rooms, lounges, retail sales, reviewing stands, skating rinks, stages, swimming pools, and other uses that would be considered to have an occupancy level greater than one person per 100 square feet (minimum square feet per occupant less than 100) pursuant to California Building Code (1998) Table 10-A.

4. Prior to recordation of a final map, issuance of building permits, or conveyance to an entity exempt from the Subdivision Map Act, the landowner shall convey an aviation easement to the County of San Bernardino as owner-operator of Chino Airport. A copy of the recorded easement shall be provided to ALUC.
5. The attached notice shall be provided to all potential purchasers of the property and all potential tenants of the buildings.
6. Any new detention basins on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
7. The project proposes primarily warehouse use with a maximum of 5,000 square feet of office within Buildings 1, 2, 3, 4, and 7 and a maximum of 10,000 square feet of office within Buildings 5 and 6, with no office use located within Compatibility Zone B1. Any proposed tenant/use or combination of proposed and existing tenants/uses within each of the buildings that exceed the office area for each building through any building permit or tenant improvement permit shall require an amended Development Review Plan approval and consultation with ALUC to verify that the building and the overall development continue to comply with the applicable average and single-acre criteria. Proposed uses that do not exceed these maximums (other than those uses previously noted in Condition 3) shall not require further Airport Land Use Commission review.
8. The open areas exhibit submitted to the Airport Land Use Commission by ~~Albert A. Webb and Associates~~ **MacDavid Aubort** dated ~~4/17/15~~ ~~7/6/15~~ **8/24/15** and included in this packet depicts **open areas** ~~area within parking areas, drive aisles, and roadways~~ as meeting open area requirements for the Major Development Plan. A minimum of 13.22 acres of open areas as defined by Countywide Policy 4.2.4 of the 2004 Riverside County Airport Land Use Compatibility Plan shall be provided on-site, of which not less than 6.042 acres shall be located within the portion of the site within Compatibility Zone B1. ~~A revised exhibit providing for this acreage without use of public rights-of-way shall be submitted to ALUC.~~ Such open areas shall have a minimum width of 75 feet and a minimum length of 300 feet, and shall not be obstructed by walls, trash enclosures, large trees or poles (light poles or other) greater than 4 inches in diameter at a height greater than 4 feet, or overhead wires. Trees or plants less than 4 inches in diameter at a height greater than 4 feet would be allowed

within the designated open area.

9. Buildings 3 and 4 located primarily within Compatibility Zone B1 shall be designed and constructed with the following risk reduction design measures. Based on these specifications, the Airport Land Use Commission recommends that the City of Eastvale adopt a 15 percent risk-reduction bonus for these structures.
 - (a) Using concrete walls for building exterior
 - (b) Enhanced fire sprinkler system that exceeds minimum building/fire code requirements – Early Suppression Fast Response (ESFR) system proposed
 - (c) Limit buildings to single-story
 - (d) Increased number of emergency exits – Building 3: 15 required, 19 proposed; Building 4: 24 required, 30 proposed

The following conditions have been added subsequent to the July 9, 2015 ALUC hearing pursuant to the terms of the FAA Obstruction Evaluation Service letter issued on June 12, 2015 for Aeronautical Study Nos. 2015-AWP-6281-OE and 2015-AWP-6286-OE and on July 31, 2015 for Aeronautical Study Nos 2015-AWP-6280-OE, 2015-AWP-6282-OE, 2015-AWP-6285-OE, and 2015-AWP-7109-OE.

10. **The Federal Aviation Administration has conducted aeronautical studies of the proposed buildings (Aeronautical Study Nos. 2015-AWP-6280-OE through 2015-AWP-6282-OE, 2015-AWP-6285-OE, 2015-AWP-6286-OE, 2015-AWP-7109-OE) and has determined that neither marking nor lighting of the structures is necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 K Change 2 and shall be maintained in accordance therewith for the life of the project.**
11. **The maximum height of the proposed buildings shall not exceed 43 feet above ground level, and the maximum elevation of the proposed buildings shall not exceed 683 feet above mean sea level, except that the maximum elevation of Building 4 may be up to 686 feet above mean sea level. Such maximum heights and elevations shall include any roof-mounted equipment.**
12. **The specific coordinates, height, and top point elevation of the proposed buildings shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in building height or elevation shall not require further review by the Airport Land Use Commission.**

13. **Temporary construction equipment used during actual construction of the buildings shall not exceed 43 feet in height, unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.**
14. **Within five (5) days after construction of each of the buildings reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned or a decision is made not to construct the applicable building.**

Guerin, John

From: Bryan Bentrott <Bryan@sumdevco.com>
Sent: Tuesday, August 25, 2015 8:09 AM
To: Guerin, John; cperring@eastvaleca.gov
Cc: 'Lee Gerry'; Bryan Bentrott
Subject: ALUC updated color graphic (The Ranch)
Attachments: OVERFLIGHTZONE_08242015F.pdf

John: I talked with Cathy yesterday evening and confirmed the attached plan is the one Taylor delivered to your office on August 10th. We made a very small change in the ALUC open space zone behind Building 4 due to the lighting levels required for this area. These lighting conclusions were a product of the photometric study we commissioned for the six (6) Phase I buildings shown on this plan.

Please contact us if you should require a meeting and/or further explanation regarding this plan. Bryan

Sincerely,

Bryan Bentrott
Principal
Direct: 949.655.8226
Cell: 949.395.2488
Email: bryan@sumdevco.com



**SUMMIT DEVELOPMENT
CORPORATION**

450 Newport Center Drive, Suite 625
Newport Beach, CA 92660-7610

From: Dan MacDavid [<mailto:dm@macdavidaubort.com>]
Sent: Thursday, August 20, 2015 2:19 PM
To: Bryan Bentrott; Donna D. Shin
Cc: Lee Gerry
Subject: ALUC updated color graphic (The Ranch)

Hi Bryan, here is the updated ALUC with color for your use.

Thank you,
Dan MacDavid
Principal



Architecture - Planning - Interiors

9281 Irvine Blvd.
Irvine, California 92618
P: 949.305.2200.224 F: 949.305.2233
Email: dm@macdavidaubort.com

LEGAL DESCRIPTION

TAKEN FROM FIRST AMERICAN TITLE INSURANCE COMPANY COMMITMENT NO. 144-010-011-011, DATED MAY 22, 2014, MAY 20, 2014 UPDATE.

THE LAND REFERRED TO IN THIS COMMITMENT IS SITUATED IN THE CITY OF IRVINE, COUNTY OF ORANGE, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS:

PARCEL 1, AS SHOWN ON PARCEL MAP #141 PER THE MAP FILED IN BOOK 31, PAGE 83 OF PARCEL MAPS, RECORDS OF IRVINE COUNTY, CALIFORNIA.

PARCEL 2, AS SHOWN ON PARCEL MAP #141 PER THE MAP FILED IN BOOK 31, PAGE 83 OF PARCEL MAPS, RECORDS OF IRVINE COUNTY, CALIFORNIA.

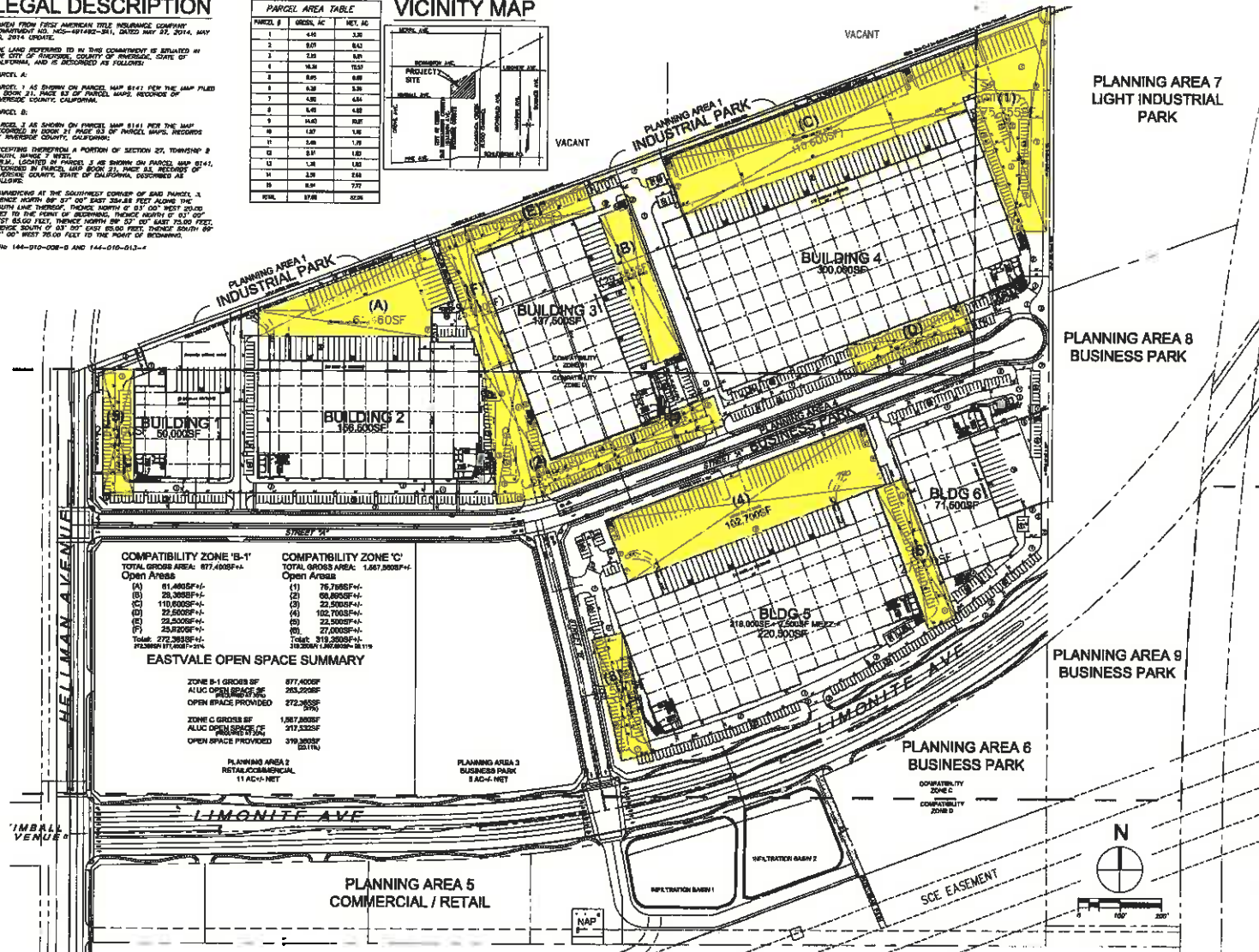
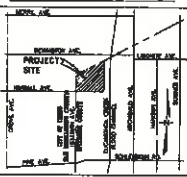
EXCEPTING THEREFROM A PORTION OF SECTION 25, TOWNSHIP 8 SOUTH, RANGE 9 WEST, S.E.1/4, LOCATED IN PARCEL 2, AS SHOWN ON PARCEL MAP #141, RECORDED IN PARCEL MAP BOOK 31, PAGE 83, RECORDS OF IRVINE COUNTY, STATE OF CALIFORNIA, DESCRIBED AS FOLLOWS:

COMMENCING AT THE SOUTHWEST CORNER OF SAID PARCEL 2, THENCE NORTH 89° 07' 00" EAST 254.88 FEET ALONG THE SOUTH LINE THEREOF, THENCE NORTH OF 87° 00' WEST 20.25 FEET TO THE POINT OF BEGINNING, THENCE NORTH 0° 07' 00" WEST 85.00 FEET, THENCE NORTH 89° 07' 00" EAST 75.00 FEET, THENCE SOUTH OF 87° 00' WEST 75.00 FEET TO THE POINT OF BEGINNING.

APN: 144-010-008-0 AND 144-010-013-4

PARCEL #	GROSS AC	NET AC
1	4.91	3.26
2	8.07	6.42
3	2.83	2.04
4	14.34	10.57
5	8.95	6.88
6	4.32	3.26
7	4.82	4.54
8	5.43	4.82
9	14.60	10.67
10	1.87	1.18
11	2.48	1.18
12	8.97	1.02
13	1.38	1.01
14	3.21	2.84
15	8.39	7.77
TOTAL	97.08	62.26

VICINITY MAP



COMPATIBILITY ZONE 'B-1'		COMPATIBILITY ZONE 'C'	
TOTAL GROSS AREA: 677,400SF +/-		TOTAL GROSS AREA: 1,567,900SF +/-	
Open Areas			
(A) 61,480SF +/-	(1) 76,788SF +/-	(G) 110,800SF +/-	(4) 102,700SF +/-
(B) 26,388SF +/-	(2) 55,800SF +/-	(H) 22,500SF +/-	(5) 22,500SF +/-
(C) 110,800SF +/-	(3) 22,500SF +/-	(I) 25,820SF +/-	(6) 27,000SF +/-
(D) 22,800SF +/-	(4) 102,700SF +/-	(J) 27,000SF +/-	Total: 319,300SF +/-
(E) 22,800SF +/-	(5) 22,500SF +/-	(K) 27,000SF +/-	Total: 319,300SF +/-
(F) 25,820SF +/-	(6) 27,000SF +/-	(L) 27,000SF +/-	Total: 319,300SF +/-
Total: 272,288SF +/-	Total: 319,300SF +/-	(M) 27,000SF +/-	Total: 319,300SF +/-
OPEN SPACE PROVIDED: 677,400SF +/-	OPEN SPACE PROVIDED: 677,400SF +/-	(N) 27,000SF +/-	Total: 319,300SF +/-
		(O) 27,000SF +/-	Total: 319,300SF +/-

EASTVALE OPEN SPACE SUMMARY

ZONE B-1 GROSS SF	677,400SF
ALLIC OPEN SPACE SF	265,200SF
OPEN SPACE PROVIDED	272,288SF
ZONE C GROSS SF	1,567,900SF
ALLIC OPEN SPACE SF	217,532SF
OPEN SPACE PROVIDED	319,300SF

PLANNING AREA 2
RETAIL/COMMERCIAL
11 AC +/- NET

PLANNING AREA 3
BUSINESS PARK
8 AC +/- NET

PLANNING AREA 5
COMMERCIAL / RETAIL

PROJECT TEAM & INFO:

CLIENT:
SUMMIT DEVELOPMENT CORPORATION
480 NEWPORT CENTER DRIVE, SUITE 825
NEWPORT BEACH, CA 92660
CONTACT: BRYAN BENTROTT (949) 688-8225
DONNA SHIM (949) 855-8225

CIVIL:
ALBERT A. WEBB ASSOCIATES
3788 MCCRY STREET
IRVINE, CA, 92618
CONTACT: MARY GONZALES
(951) 387-1323 X 21

ARCHITECT/PLANNING:
MacDAVID ALBORT AND ASSOC.
1281 Irvine Boulevard
Irvine, CALIFORNIA 92618
Contact: Dan MacDAVID
949-305-2200 x224

LANDSCAPE ARCHITECT:
RIDGE LANDSCAPE ARCHITECTS
8841 RESEARCH DR. # 200
IRVINE, CA 92618
CONTACT: JIM RIDGE (949) 387-1323 X 21
(949) 387-1323 X 21

PROJECT ADDRESS:
LIMONITE AVE
EASTVALE, CALIFORNIA
APN: 144-010-008 144-010-013

GENERAL PLAN DESIGNATION = LOCATED IN EASTVALE AREA PLAN OF GENERAL PLAN
ZONE = LIGHT INDUSTRIAL & BUSINESS PARK

SITE PARKING ANALYSIS

BUILDING #	OFF. / RETAIL / STORAGE / TOTAL STALLS PROVIDED (PROVIDED)	BUILDING #	OFF. / RETAIL / STORAGE / TOTAL STALLS PROVIDED (PROVIDED)
BUILDING 1	OFF. - 4,000SF - 24 STALLS TOTAL: 24 STALLS PROVIDED (1,100 REQUIRED)	BUILDING 4	OFF. - 4,000SF - 24 STALLS TOTAL: 24 STALLS PROVIDED (1,100 REQUIRED)
BUILDING 2	OFF. - 4,000SF - 24 STALLS TOTAL: 24 STALLS PROVIDED (1,100 REQUIRED)	BUILDING 5	OFF. - 4,000SF - 24 STALLS TOTAL: 24 STALLS PROVIDED (1,100 REQUIRED)
BUILDING 3	OFF. - 4,000SF - 24 STALLS TOTAL: 24 STALLS PROVIDED (1,100 REQUIRED)	BUILDING 6	OFF. - 4,000SF - 24 STALLS TOTAL: 24 STALLS PROVIDED (1,100 REQUIRED)

TOTAL PARKING REQUIRED: 1,048 STALLS (REQ.)
TOTAL PARKING PROVIDED: 1,088 STALLS (PROV.)

SITE DATA TABLE							
BUILDING	LAND AREA	1ST FLR. (SF)	MEZZ. (SF)	BUILDING (SF)	F.A.R.	LIENCRP %	PARKING STALLS
BUILDING 1	11,300SF +/-	47,000SF	3,000SF	50,000SF	34.5%	10.0%	40 STALLS (1,200 REQ.)
BUILDING 2	12,400SF +/-	134,000SF	2,800SF	136,800SF	42.7%	10.0%	148 + 20P+ 175 STALLS (1,100 REQ.)
BUILDING 3	27,800SF +/-	178,000SF	2,500SF	180,500SF	46.4%	11.7%	128 + 20P+ 148 STALLS (1,200 REQ.)
BUILDING 4	12,400SF +/-	377,000SF	3,500SF	380,500SF	44.2%	11.0%	128 + 20P+ 148 STALLS (1,200 REQ.)
BUILDING 5	11,300SF +/-	218,000SF	2,800SF	220,800SF	43.8%	13.4%	100 + 20P+ 120 STALLS (1,100 REQ.)
BUILDING 6	12,400SF +/-	80,000SF	2,800SF	82,800SF	40.2%	15.8%	85 STALLS (1,200 REQ.)
TOTAL	87,600SF +/-	881,000SF	18,000SF	899,000SF	42.5%	12.4%	786 STALLS (1,170 REQ.)

- KEY NOTES:**
- ① ASPHALT DRIVE
 - ② CONCRETE CURB
 - ③ LANDSCAPE AREA
 - ④ PAVING (SEE LANDSCAPE)
 - ⑤ STANDARD PARKING STALL
 - ⑥ ACCESSIBLE PARKING
 - ⑦ 6' HEIGHT CONC. SCREEN WALL
 - ⑧ ROLLING GATE
 - ⑨ CONC. TRUCK DOCKING AREA
 - ⑩ TRASH ENCLOSURE
 - ⑪ 6' HEIGHT TUBULAR STL. FENCE
 - ⑫ 42" GUARD RAIL
 - ⑬ 6' HEIGHT TUBULAR STL. FENCE



the ranch at Eastvale

A.L.U.C. Over Flight Master Plan

Eastvale, California

CLIENT: SUMMIT DEVELOPMENT CORPORATION
DRAWING DATE: 08/24/2015

A0.0

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 2601 Meacham Boulevard
 Fort Worth, TX 76193

Aeronautical Study No.
 2015-AWP-6280-OE

Issued Date: 07/31/2015

Bryan Bentrott or Taylor Gerry
 Summit Development Corporation
 450 Newport Center Drive
 Suite 625
 Newport Beach, CA 92660

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building Building 2
 Location: Eastvale, CA
 Latitude: 33-58-18.77N NAD 83
 Longitude: 117-36-34.29W
 Heights: 639 feet site elevation (SE)
 43 feet above ground level (AGL)
 682 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed and maintained in accordance with FAA Advisory circular 70/7460-1 K Change 2.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/31/2017 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates , heights, frequency(ies) and power . Any changes in coordinates , heights, and frequencies or use of greater power will void this determination. Any future construction or alteration , including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

If we can be of further assistance, please contact our office at (310) 725-6557. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2015-AWP-6280-OE.

Signature Control No: 253409869-259378249

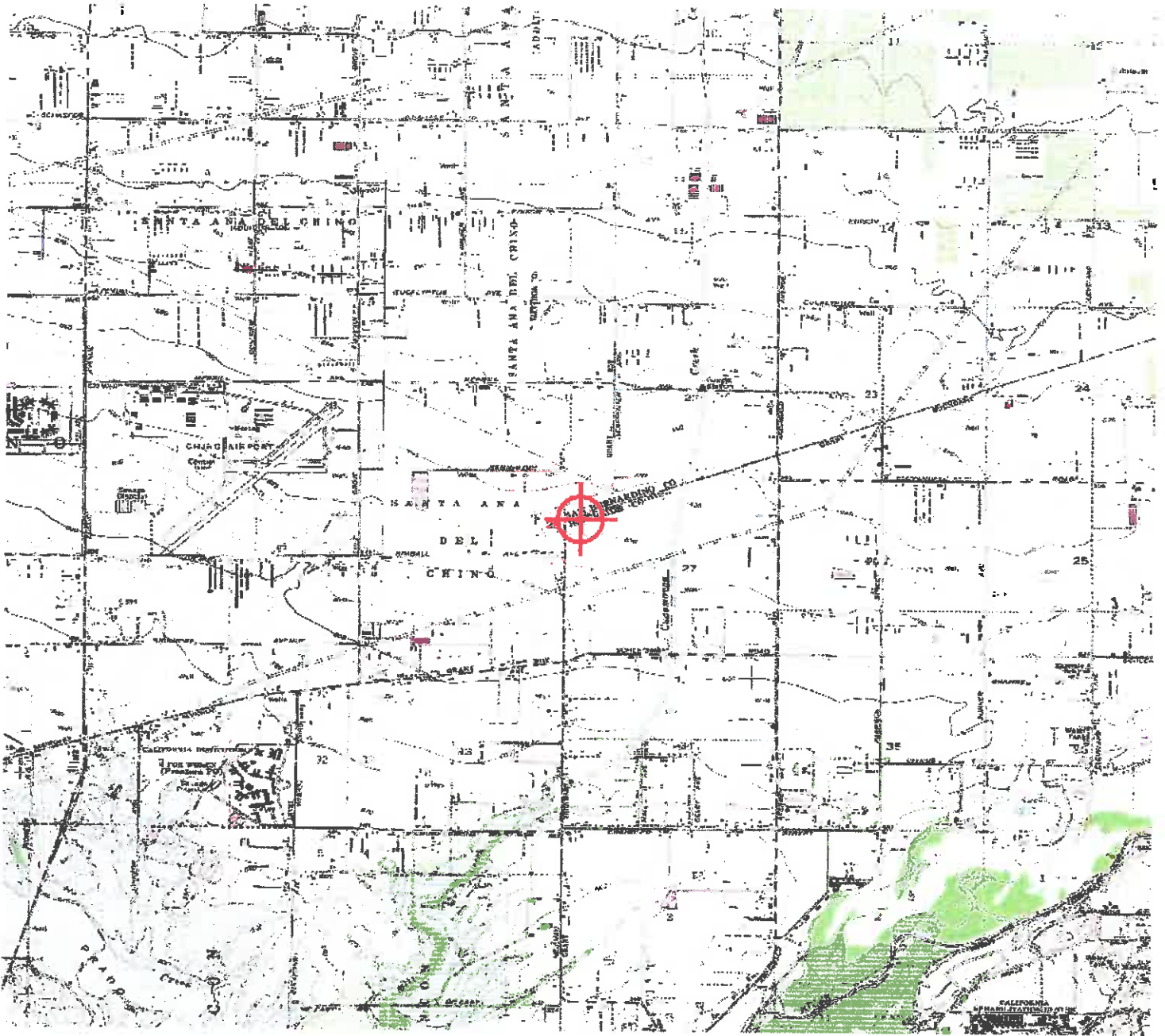
(DNE)

Karen McDonald
Specialist

Attachment(s)
Case Description
Map(s)

Case Description for ASN 2015-AWP-6280-OE

Building is one of 7 to be constructed on the 57 acres during the first phase of development. The data submitted is for the closest point to the nearest runway at CNO and is for the highest point on the building.





Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
2601 Meacham Boulevard
Fort Worth, TX 76193

Aeronautical Study No.
2015-AWP-6281-OE

Issued Date: 06/12/2015

Bryan Bentrott or Taylor Gerry
Summit Development Corporation
450 Newport Center Drive
Suite 625
Newport Beach, CA 92660

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building Building 3
Location: Eastvale, CA
Latitude: 33-58-21.33N NAD 83
Longitude: 117-36-28.10W
Heights: 637 feet site elevation (SE)
43 feet above ground level (AGL)
680 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed and maintained in accordance with FAA Advisory circular 70/7460-1 K Change 2.

This determination expires on 12/12/2016 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates , heights, frequency(ies) and power . Any changes in coordinates , heights, and frequencies or use of greater power will void this determination. Any future construction or alteration , including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

If we can be of further assistance, please contact our office at (310) 725-6557. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2015-AWP-6281-OE.

Signature Control No: 253409870-255063211

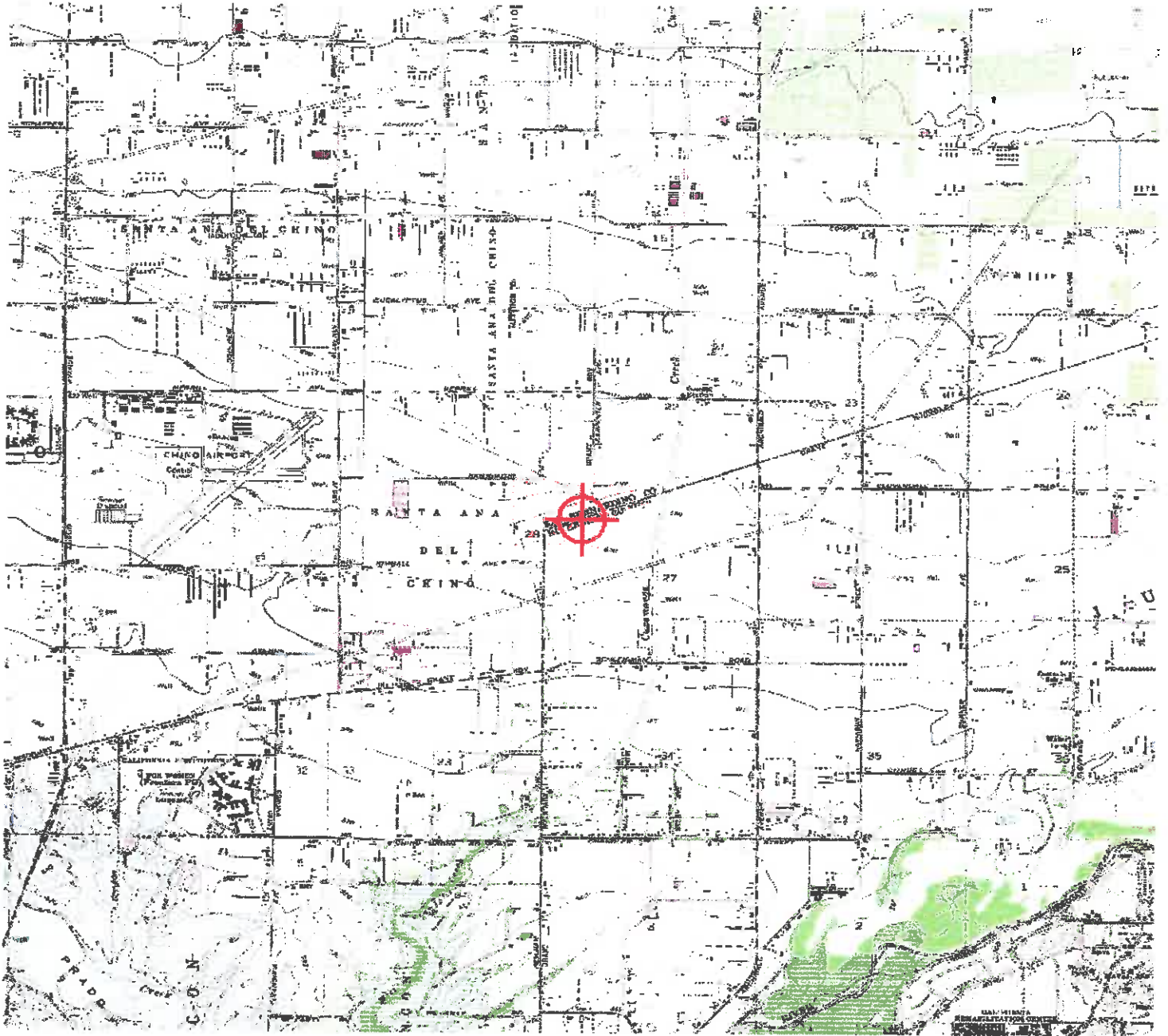
(DNE)

Karen McDonald
Specialist

Attachment(s)
Case Description
Map(s)

Case Description for ASN 2015-AWP-6281-OE

Building is one of 7 to be constructed on the 57 acres during the first phase of development. The data submitted is for the closest point to the nearest runway at CNO and is for the highest point on the building.





Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
2601 Meacham Boulevard
Fort Worth, TX 76193

Aeronautical Study No.
2015-AWP-6282-OE

Issued Date: 07/31/2015

Bryan Bentrott or Taylor Gerry
Summit Development Corporation
450 Newport Center Drive
Suite 625
Newport Beach, CA 92660

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building Building 4
Location: Eastvale, CA
Latitude: 33-58-22.10N NAD 83
Longitude: 117-36-22.77W
Heights: 643 feet site elevation (SE)
43 feet above ground level (AGL)
686 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed and maintained in accordance with FAA Advisory circular 70/7460-1 K Change 2.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/31/2017 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates , heights, frequency(ies) and power . Any changes in coordinates , heights, and frequencies or use of greater power will void this determination. Any future construction or alteration , including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

If we can be of further assistance, please contact our office at (310) 725-6557. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2015-AWP-6282-OE.

Signature Control No: 253409871-259378251

(DNE)

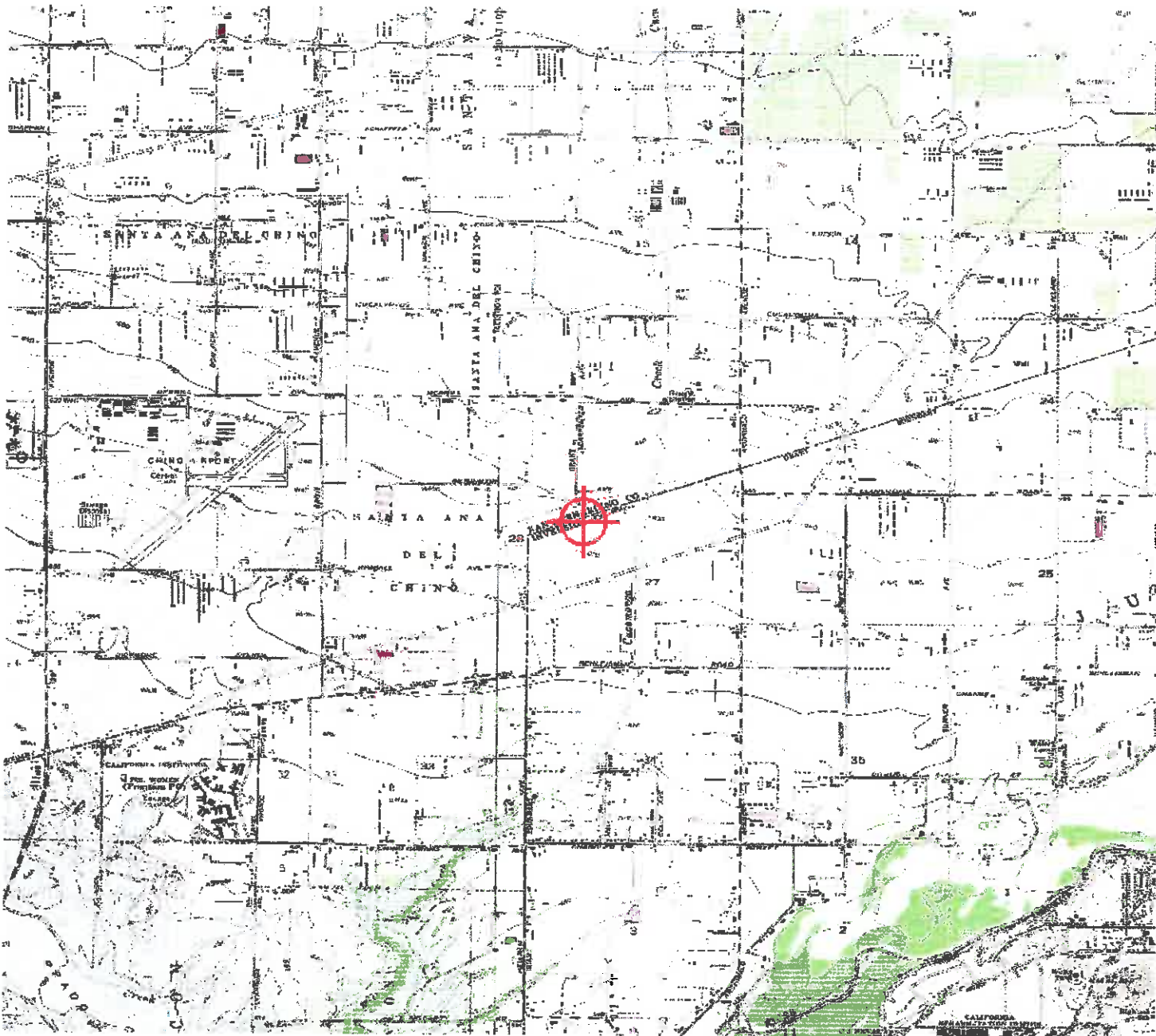
Karen McDonald
Specialist

Attachment(s)
Case Description
Map(s)

Case Description for ASN 2015-AWP-6282-OE

Building is one of 7 to be constructed on the 57 acres during the first phase of development. The data submitted is for the closest point to the nearest runway at CNO and is for the highest point on the building.

TOPO Map for ASN 2015-AWP-6282-OE





Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
2601 Meacham Boulevard
Fort Worth, TX 76193

Aeronautical Study No.
2015-AWP-6285-OE

Issued Date: 07/31/2015

Bryan Bentrott or Taylor Gerry
Summit Development Corporation
450 Newport Center Drive
Suite 625
Newport Beach, CA 92660

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building Building 7
Location: Eastvale, CA
Latitude: 33-58-17.08N NAD 83
Longitude: 117-36-16.16W
Heights: 639 feet site elevation (SE)
43 feet above ground level (AGL)
682 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed and maintained in accordance with FAA Advisory circular 70/7460-1 K Change 2.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/31/2017 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates , heights, frequency(ies) and power . Any changes in coordinates , heights, and frequencies or use of greater power will void this determination. Any future construction or alteration , including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

If we can be of further assistance, please contact our office at (310) 725-6557. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2015-AWP-6285-OE.

Signature Control No: 253409874-259378250

Karen McDonald
Specialist

(DNE)

Attachment(s)

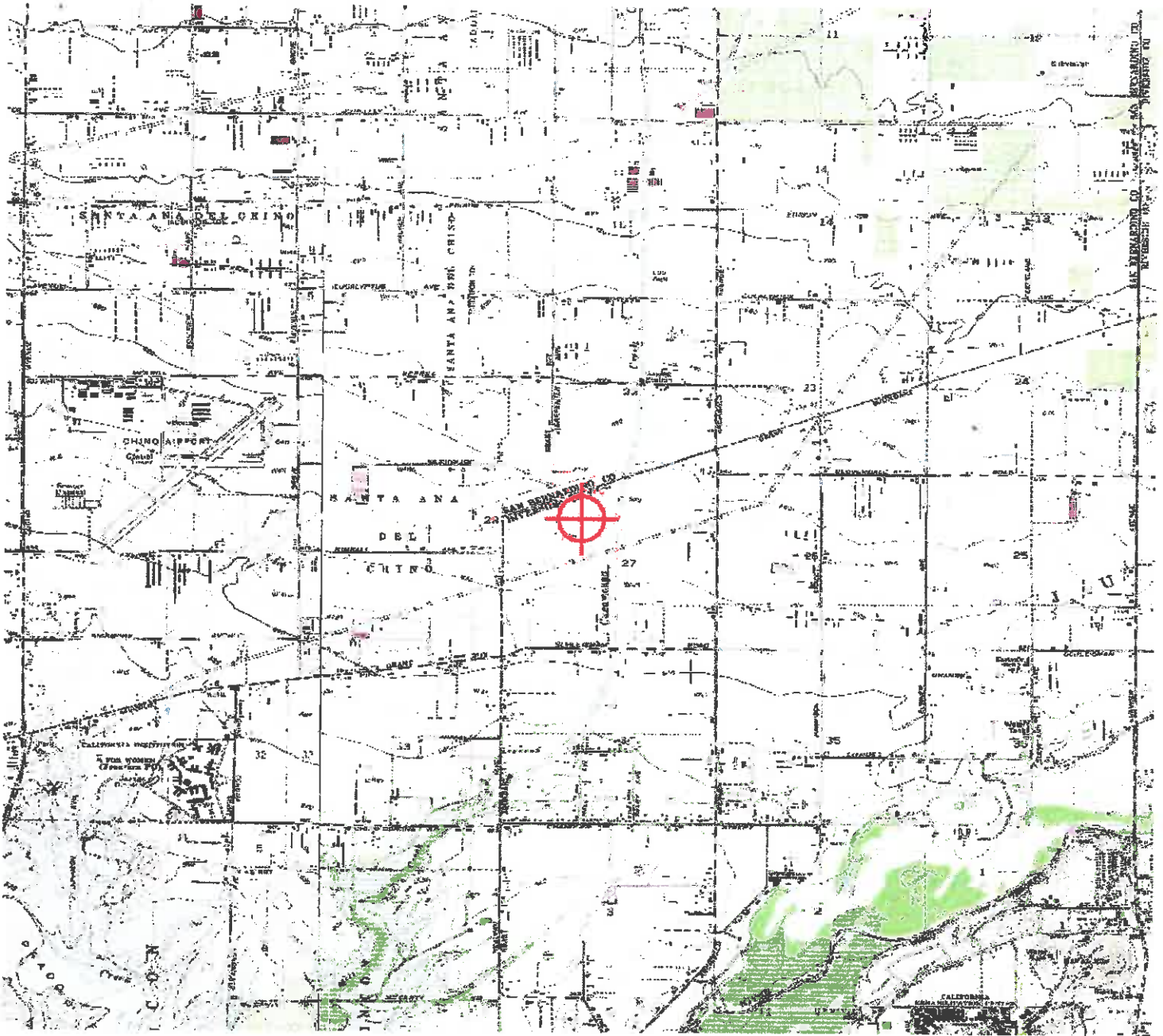
Case Description

Map(s)

Case Description for ASN 2015-AWP-6285-OE

Building is one of 7 to be constructed on the 57 acres during the first phase of development. The data submitted is for the closest point to the nearest runway at CNO and is for the highest point on the building.

TOPO Map for ASN 2015-AWP-6285-OE





Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 2601 Meacham Boulevard
 Fort Worth, TX 76193

Aeronautical Study No.
 2015-AWP-6286-OE

Issued Date: 06/12/2015

Bryan Bentrott or Taylor Gerry
 Summit Development Corporation
 450 Newport Center Drive
 Suite 625
 Newport Beach, CA 92660

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building Building 1
 Location: Eastvale, CA
 Latitude: 33-58-18.17N NAD 83
 Longitude: 117-36-37.81W
 Heights: 640 feet site elevation (SE)
 43 feet above ground level (AGL)
 683 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed and maintained in accordance with FAA Advisory circular 70/7460-1 K Change 2.

This determination expires on 12/12/2016 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates , heights, frequency(ies) and power . Any changes in coordinates , heights, and frequencies or use of greater power will void this determination. Any future construction or alteration , including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

If we can be of further assistance, please contact our office at (310) 725-6557. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2015-AWP-6286-OE.

Signature Control No: 253409875-255063212

(DNE)

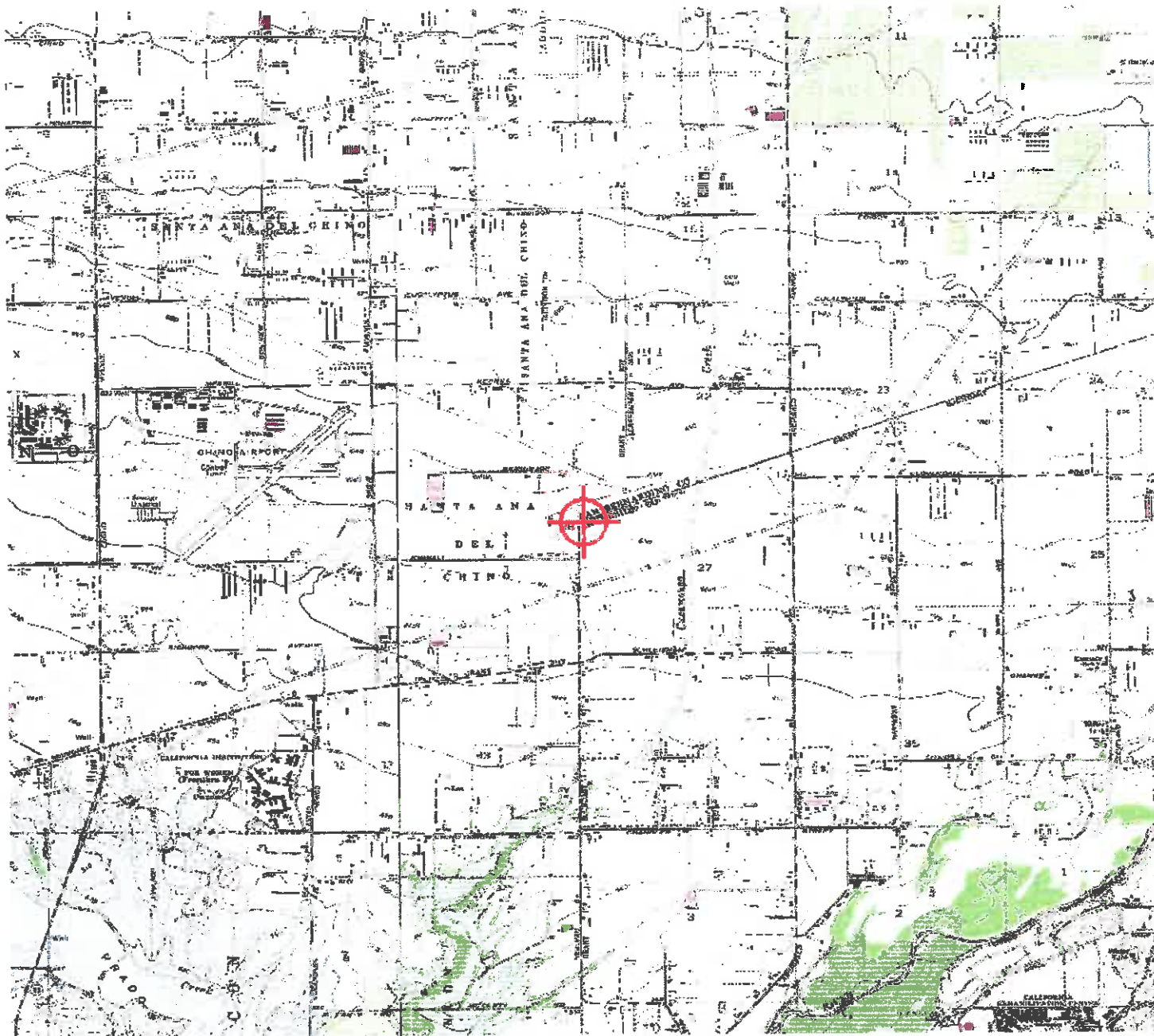
Karen McDonald
Specialist

Attachment(s)
Case Description
Map(s)

Case Description for ASN 2015-AWP-6286-OE

Building is one of 7 to be constructed on the 57 acres during the first phase of development. The data submitted is for the closest point to the nearest runway at CNO and is for the highest point on the building.

TOPO Map for ASN 2015-AWP-6286-OE





Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
2601 Meacham Boulevard
Fort Worth, TX 76193

Aeronautical Study No.
2015-AWP-7109-OE

Issued Date: 08/03/2015

Bryan Bentrott or Taylor Gerry
Summit Development Corporation
450 Newport Center Drive
Suite 625
Newport Beach, CA 92660

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building Building 5 Revised
Location: Eastvale, CA
Latitude: 33-58-13.42N NAD 83
Longitude: 117-36-24.26W
Heights: 636 feet site elevation (SE)
43 feet above ground level (AGL)
679 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed and maintained in accordance with FAA Advisory circular 70/7460-1 K Change 2.

This determination expires on 02/03/2017 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates , heights, frequency(ies) and power . Any changes in coordinates , heights, and frequencies or use of greater power will void this determination. Any future construction or alteration , including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

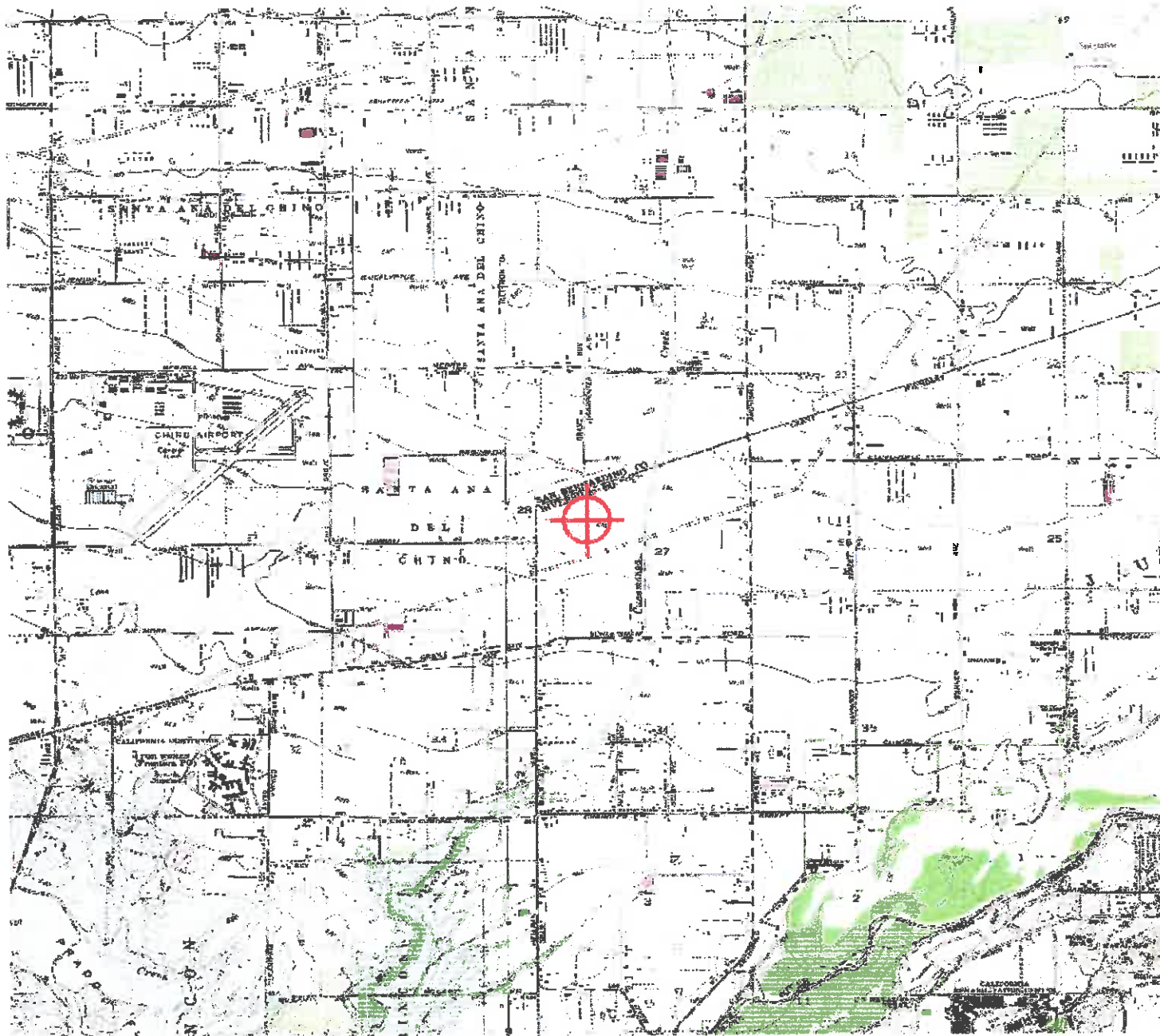
If we can be of further assistance, please contact our office at (425) 227-2625. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2015-AWP-7109-OE.

Signature Control No: 256908425-259466413
Paul Holmquist
Technician

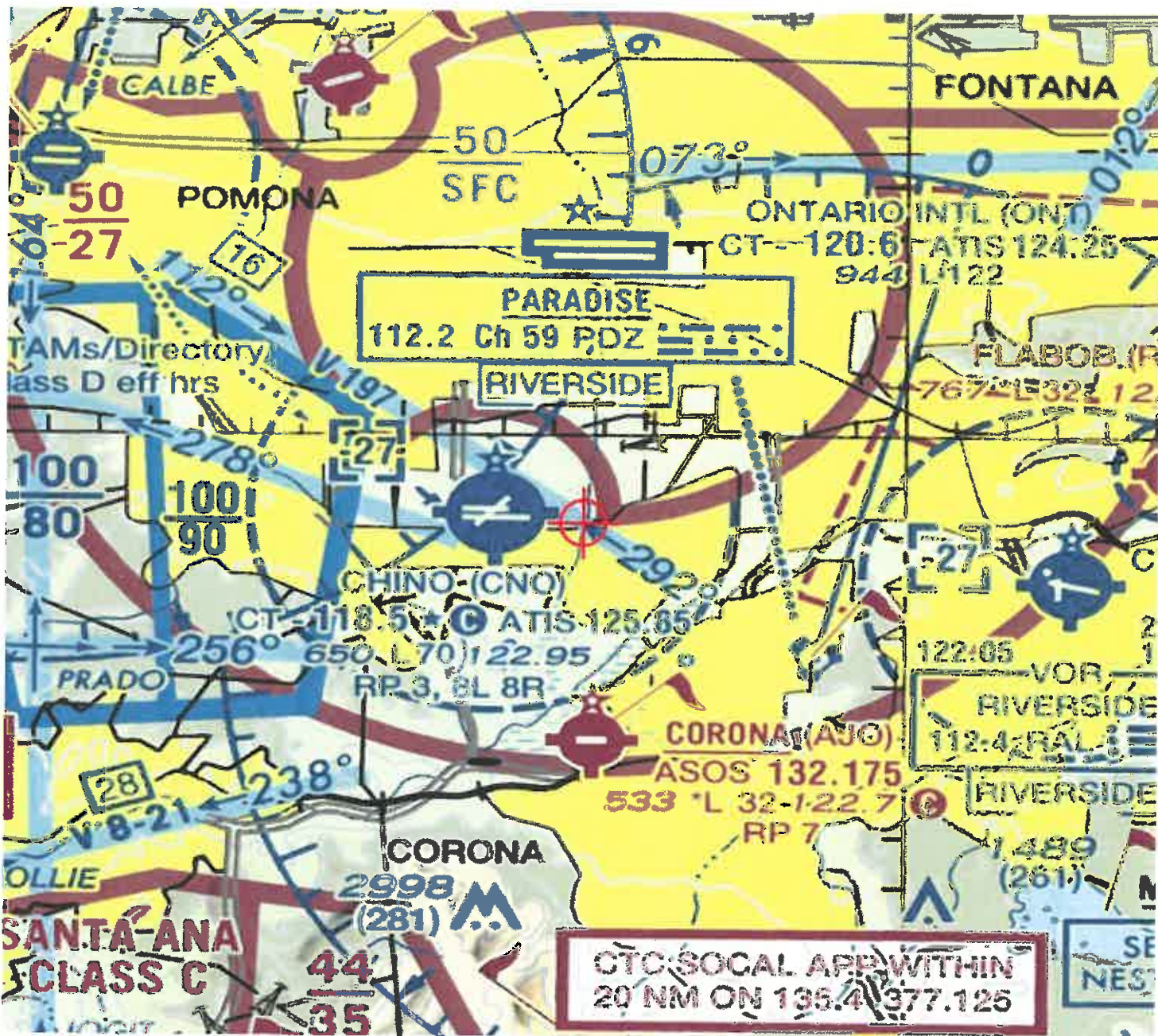
(DNE)

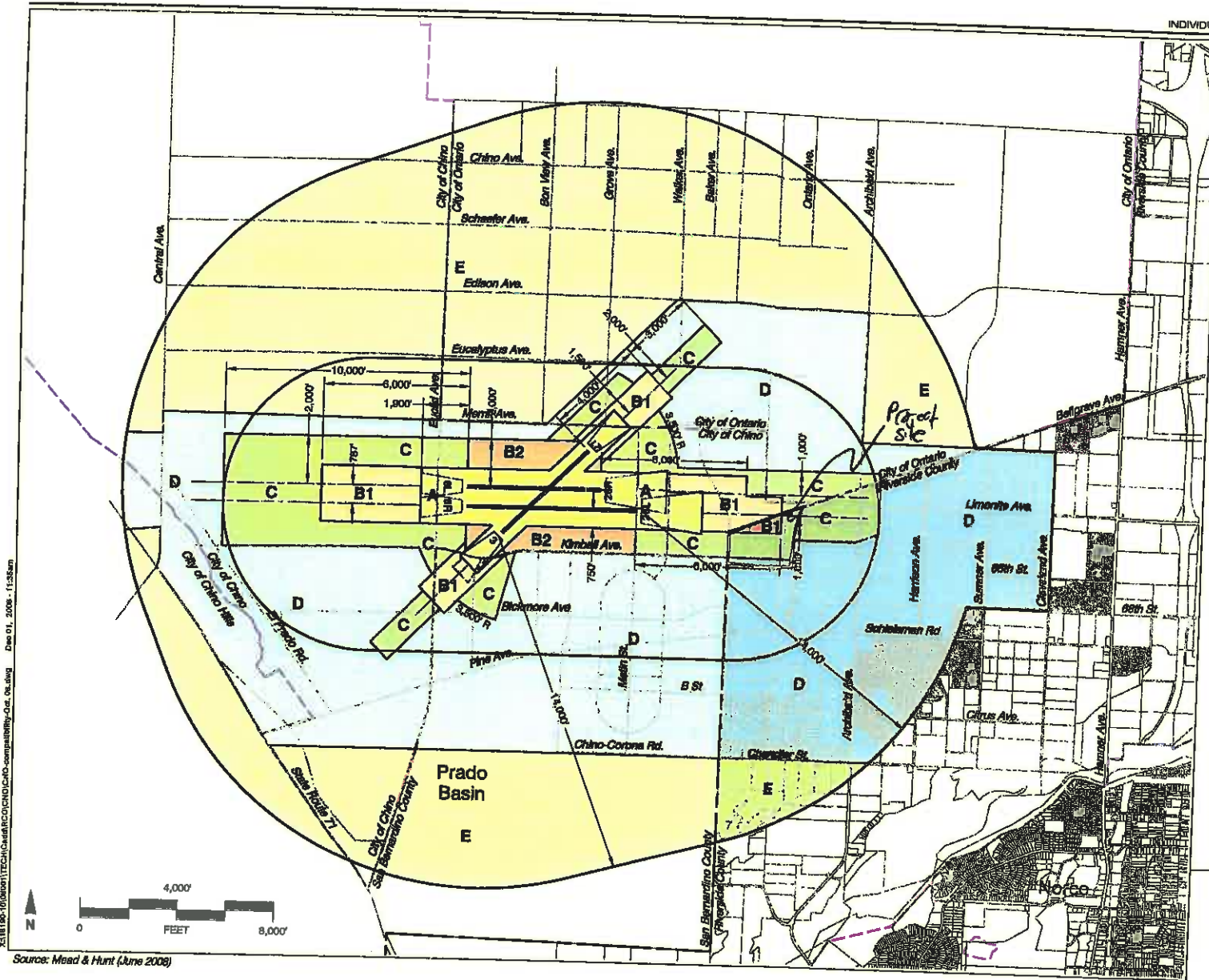
Attachment(s)
Map(s)

TOPO Map for ASN 2015-AWP-7109-OE



Sectional Map for ASN 2015-AWP-7109-OE





Legend

- Compatibility Zones**
- Airport Influence Area Boundary*
 - Zone A
 - Zone B1
 - Zone B2
 - Zone C
 - Zone D
 - Zone E
- Boundary Lines**
- Airport Property Line
 - City Limits
 - County Line

Note
 Airport influence boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

See Chapter 2, Table 2A, and the Additional Compatibility Policies for Chino Airport for compatibility criteria associated with this map.

*The policies in this plan apply only to the portions of the airport influence area lying within Riverside County. Compatibility Zones in San Bernardino County are shown only to provide context for the Riverside County area.

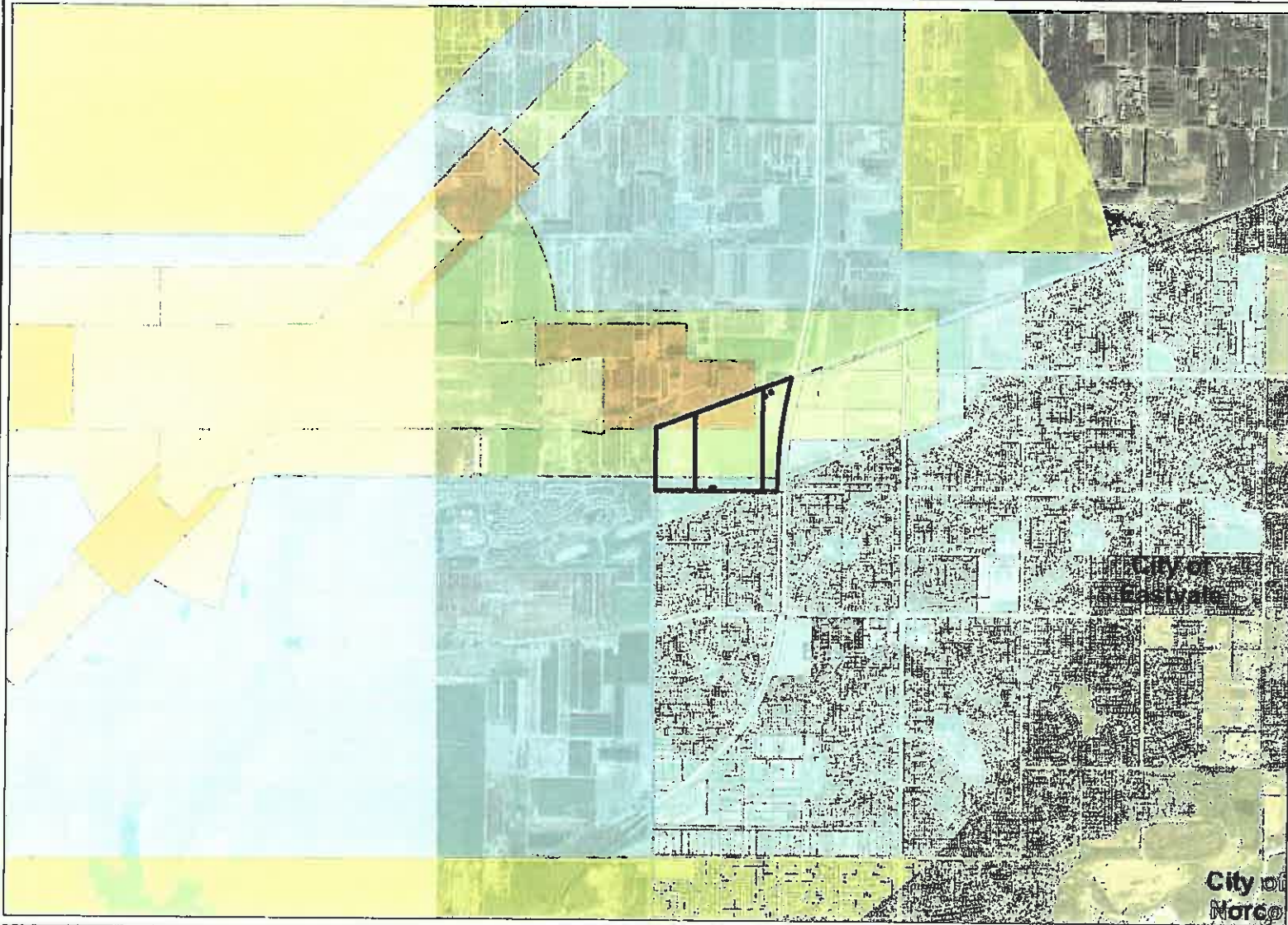
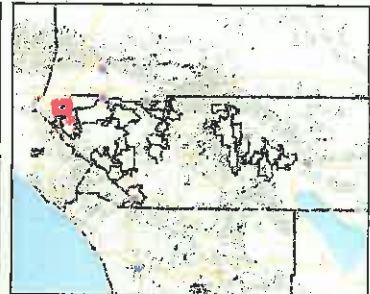
Riverside County
Airport Land Use Commission
Riverside County
Airport Land Use Compatibility Plan
Policy Document
 (Adopted September 2008)

Map CH-1

Compatibility Map
Chino Airport

X:\18180-10\08001\TECH\CH3\CH3-Compatibility-02.dwg Dec 01, 2008 - 11:58am
 Source: Mead & Hunt (June 2008)

My Map



Legend

Airport Compatibility

- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT
- C2-HIGHT-EXC1
- C2-HIGHT-EXC5



0 4,030 8,059 Feet



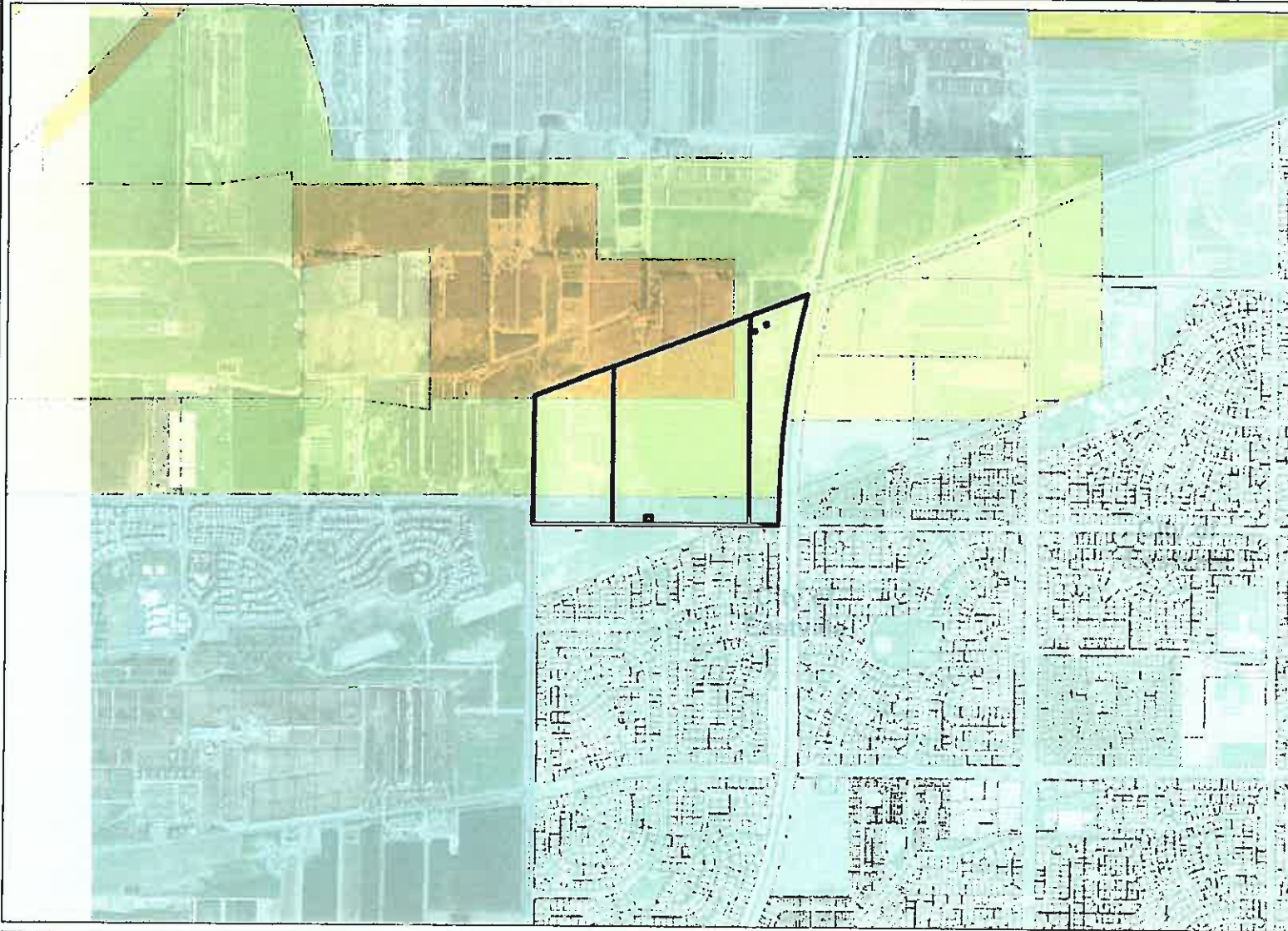
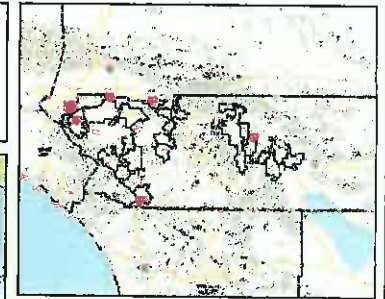
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Notes

My Map



Legend

Airport Compatibility

- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT
- C2-HIGHT-EXC1
- C2-HIGHT-EXC5



0 2,015 4,030 Feet



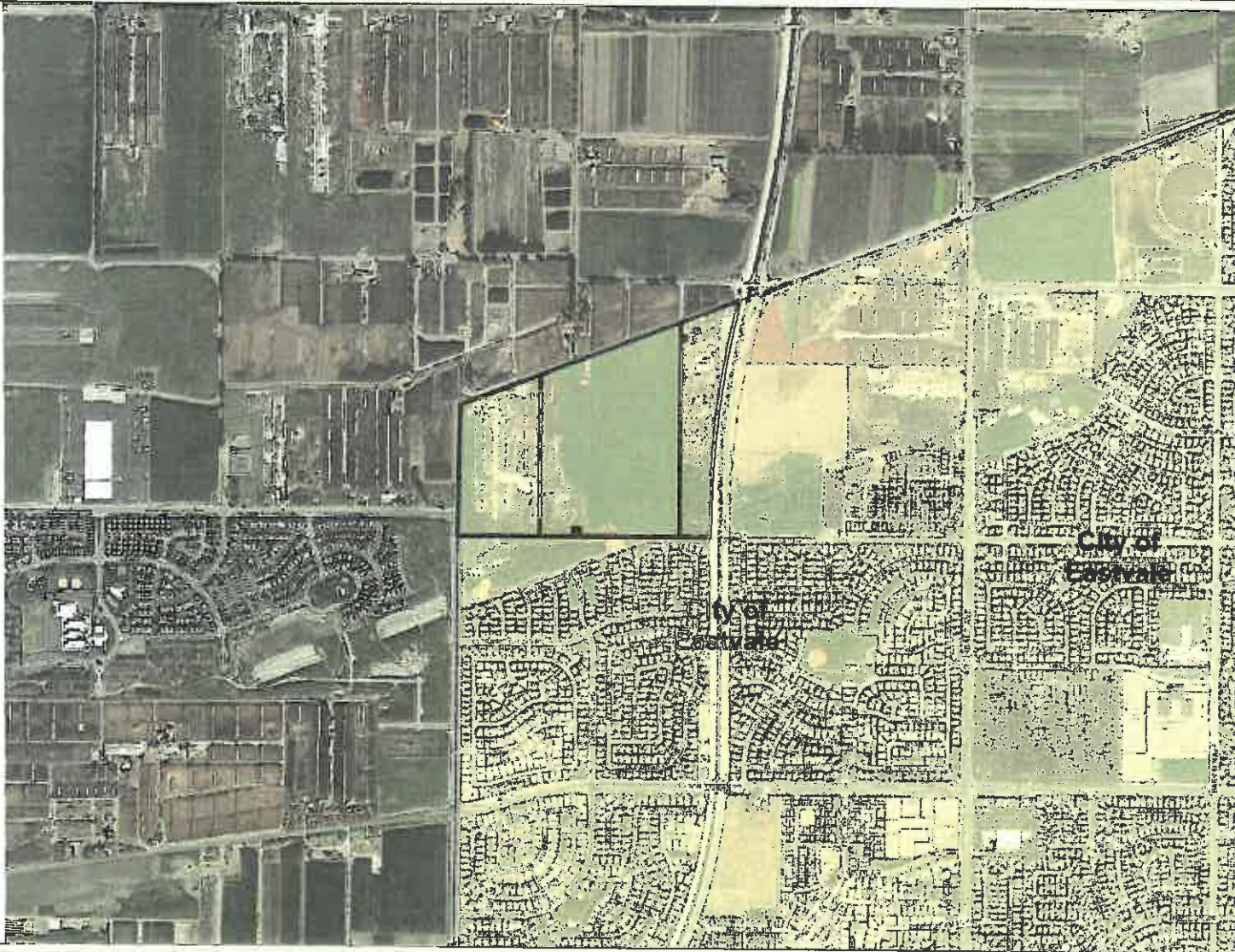
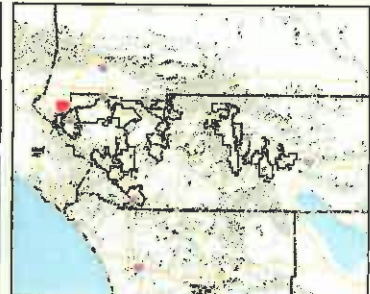
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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Notes

My Map



Legend

- City Boundaries
- Cities
- roads**
- roadsanno**
- highways**
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads**
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities
- hydrographyllines**
- waterbodies**
- Lakes
- Rivers



0 2,015 4,030 Feet

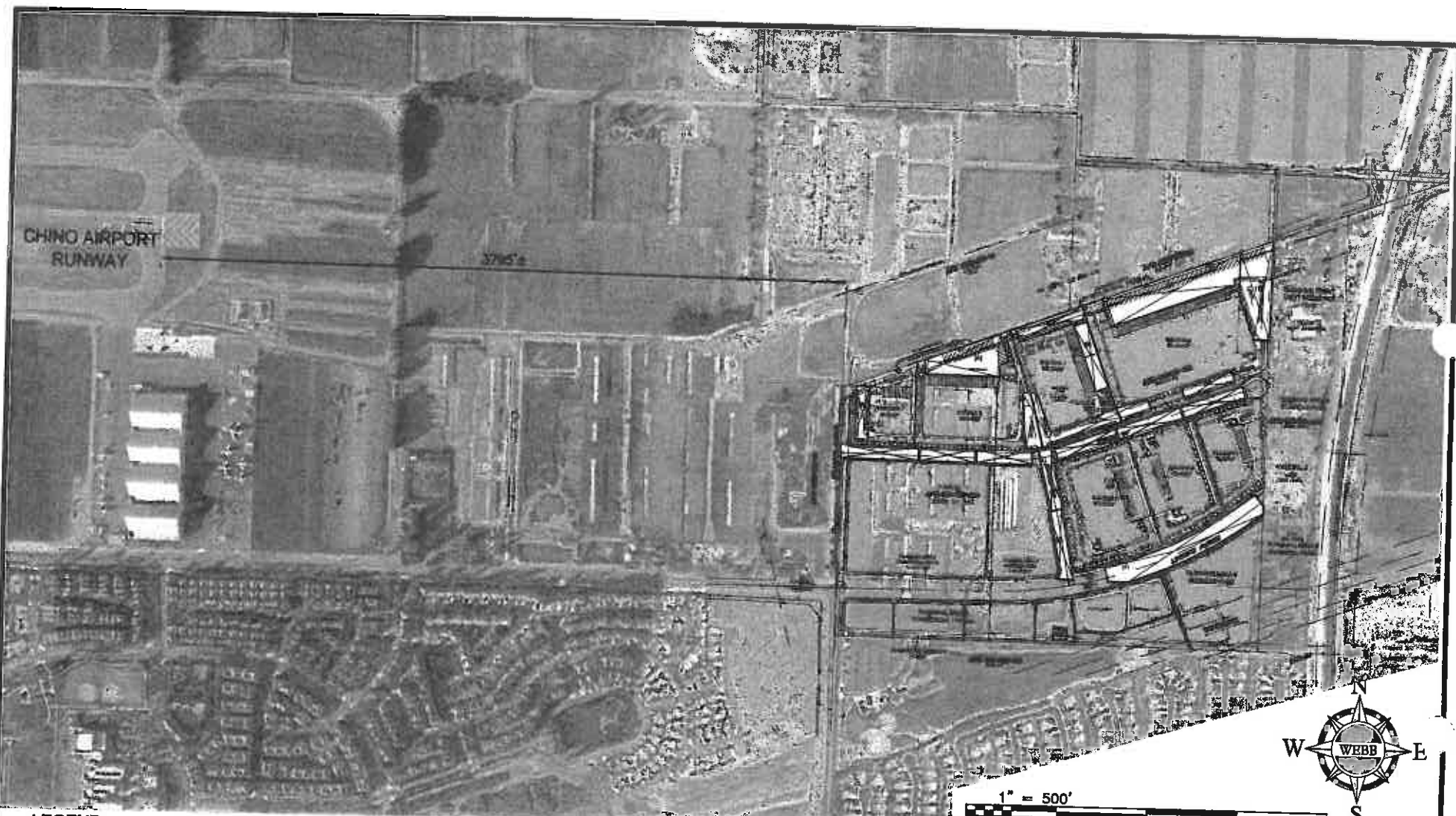


IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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Notes



LEGEND:

- — — — — PROPERTY LINE
- EMERGENCY LANDING
- ⊙ PROPOSED STREET LIGHT

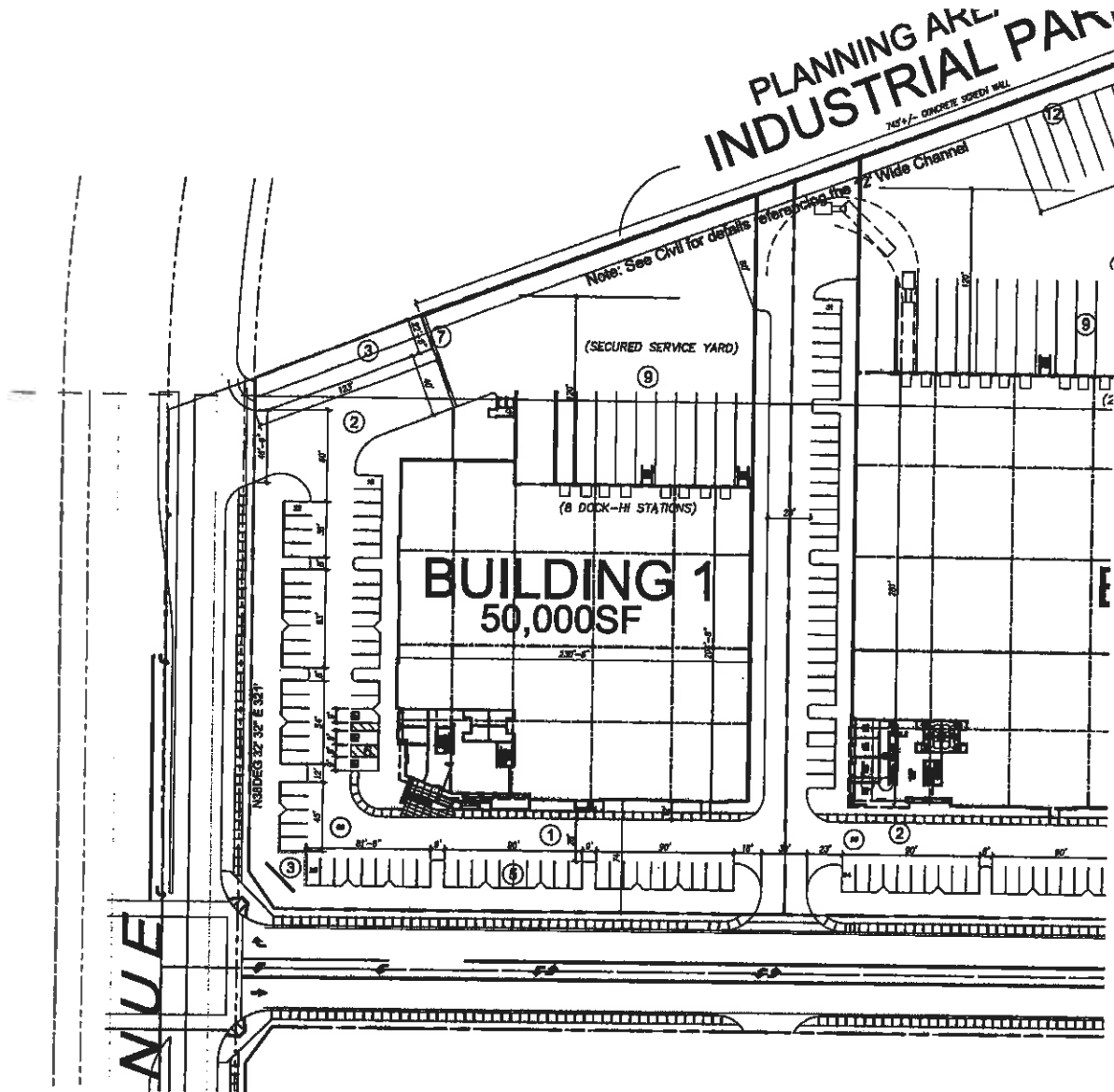
CHINO AIRPORT RUNWAY TO HELLMAN AVE. DISTANCE

SCALE:
DATE:
DESIGNED:
CHECKED:
PLN CK REF:
F.B.

ALBERT A. WEBB ASSOCIATES
 ENGINEERING CONSULTANTS
 3788 McCRAY STREET
 RIVERSIDE CA. 92506
 PH. (951) 886-1070
 FAX (951) 788-1256

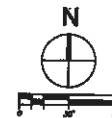
w.o. 14-0157
 SHEET
1
 OF 1 SHEETS

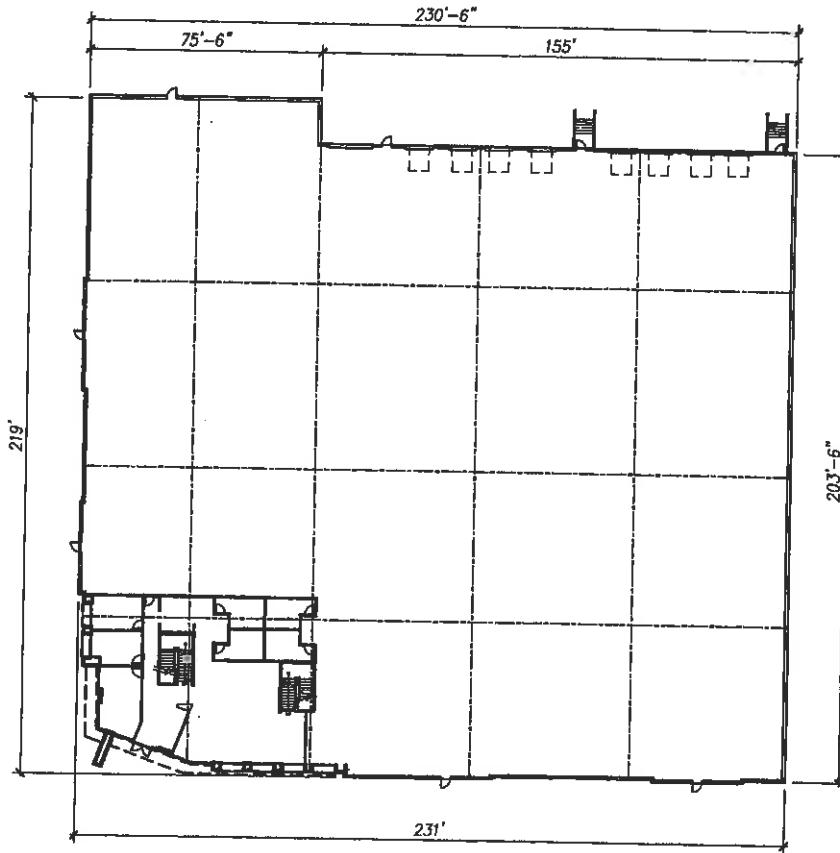
PLANNING AREA INDUSTRIAL PARK



SITE DATA TABLE							
BUILDING:	LAND AREA:	1ST FLR. (S.F.)	MEZZ. (S.F.)	BUILDING (S.F.)	F.A.R. %	LANDSCP %	PARKING STALLS
BUILDING 1	143,720SF	47,500SF	2,800SF	50,000SF	34.8%	10.0%	68 STALLS (1.36/1000)
BUILDING 2	366,860SF	154,000SF	2,800SF	168,800SF	42.7%	10.0%	146 STALLS (.65/1000)
BUILDING 3	286,292SF	136,000SF	2,800SF	137,800SF	48.4%	11.7%	138 STALLS (1/1000)
BUILDING 4	677,364SF	297,800SF	2,800SF	300,000SF	44.3%	11.0%	316 STALLS (1.05/1000)
BUILDING 5	291,851SF	121,000SF	2,800SF	123,800SF	42.3%	16.8%	172 STALLS (1.47/1000)
BUILDING 6	228,244SF	102,800SF	2,500SF	105,000SF	45.8%	12.7%	116 STALLS (1.1/1000)
BUILDING 7	175,576SF	70,000SF	2,500SF	72,500SF	41.3%	16.9%	96 STALLS (1.31/1000)
TOTAL	2,161,114SF	827,900SF	17,500SF	945,000SF	43.3%	12.6%	1,050 STALLS (1.11/1000)

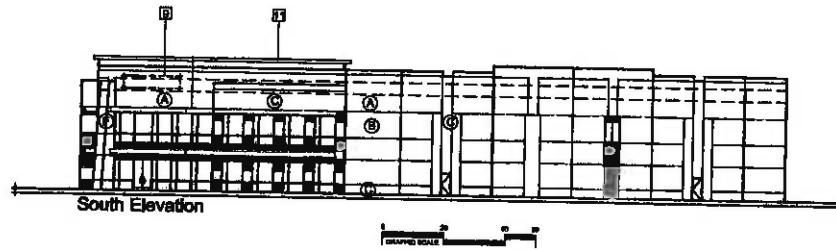
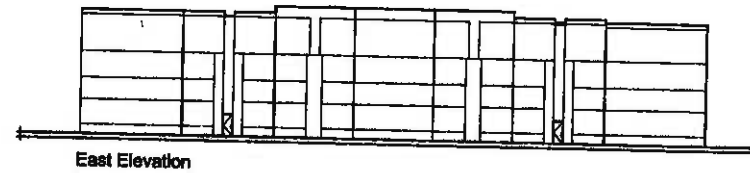
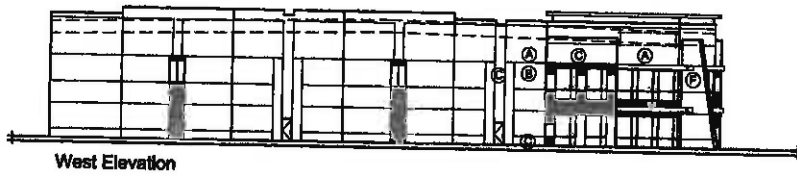
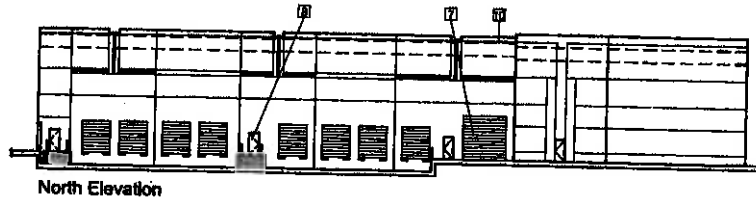
- KEY NOTES:**
- ① ASPHALT DRIVE
 - ② CONCRETE CURB
 - ③ LANDSCAPE AREA
 - ④ PAVING (SEE LANDSCAPE)
 - ⑤ STANDARD PARKING STALL
 - ⑥ ACCESSIBLE PARKING
 - ⑦ 9' HEIGHT CONC. SCREEN WALL
 - ⑧ ROLLING GATE
 - ⑨ CONC. TRUCK DOCKING AREA
 - ⑩ TRASH ENCLOSURE
 - ⑪ 8' HEIGHT TUBULAR STL. FENCE
 - ⑫ 42" GUARD RAIL
 - ⑬ 6' HEIGHT TUBULAR STL. FENCE





COLOR LEGEND:

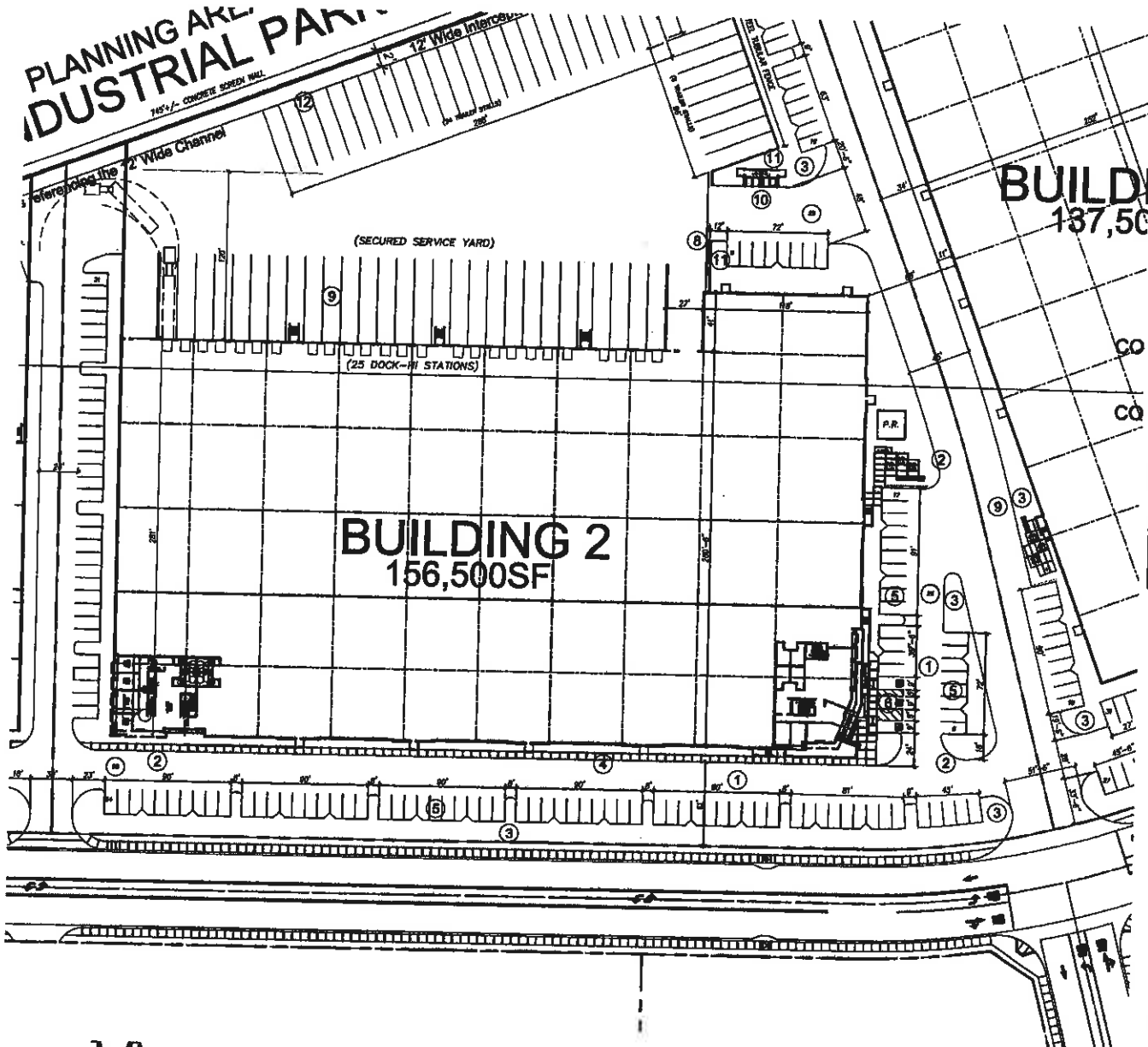
① VISTA LIGHT CREAM EP64	④ VISTA BASE B96
② VISTA MEDIUM CREAM EP71	⑤ VISTA RED CLAY B811
③ VISTA BROWN B95	⑥ VISTA TAUPE EP68



KEY NOTES:

① CONCRETE TILT UP PANEL	⑦ GRADE DOOR
② MEDIUM PERFORMANCE GLASS	⑧ SIGN DOOR
③ CLEAR ANODIZED ALUMINUM STORMDOOR	⑨ TENANT SIGNAGE
④ CLEAR ANODIZED ALUMINUM CANOPY	⑩ LINE OF ROOF BEYOND
⑤ CONCRETE FORMWORK	⑪ ALUMINUM CLAD CORNER
⑥ 2" WIDE REVEAL X 1-60" DEEP	⑫ NA
	⑬ NA

PLANNING AREA
INDUSTRIAL PARK



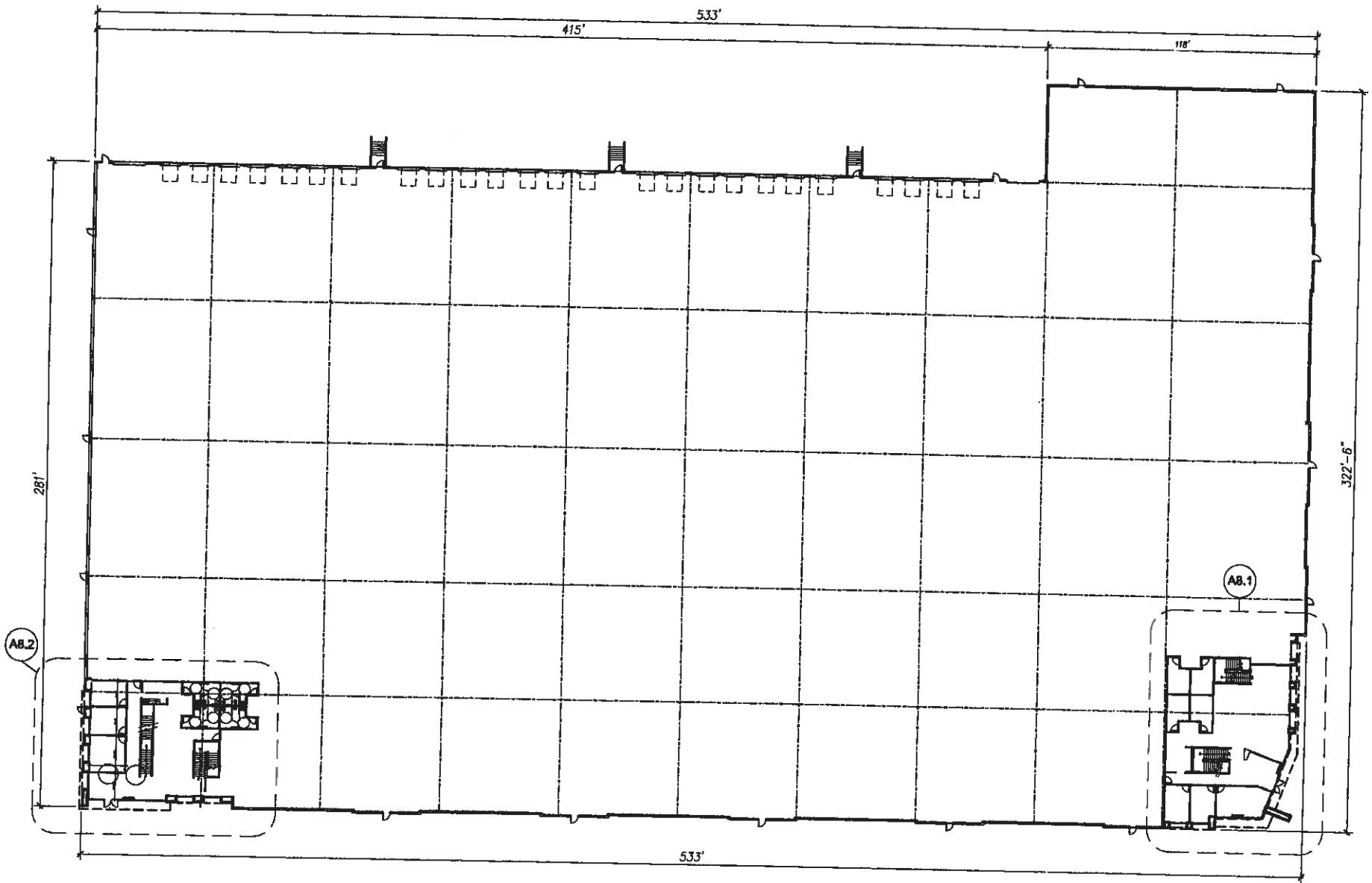
SITE DATA TABLE							
BUILDING	LAND AREA	1ST FLR (S.F.)	MEZZ (S.F.)	BUILDING (S.F.)	P.A.R. %	LANDSCP %	PARKING STALLS
BUILDING 1	143,720SF	47,800SF	2,500SF	60,000SF	34.8%	10.0%	88 STALLS (1,261/1000)
BUILDING 2	306,800SF	184,000SF	2,500SF	169,500SF	42.7%	10.0%	146 STALLS (1,631/1000)
BUILDING 3	208,282SF	136,000SF	2,500SF	137,500SF	48.4%	11.7%	138 STALLS (1/1000)
BUILDING 4	677,384SF	297,500SF	2,500SF	300,000SF	44.5%	11.8%	318 STALLS (1,061/1000)
BUILDING 5	281,991SF	121,000SF	2,500SF	123,500SF	42.3%	18.9%	172 STALLS (1,41/1000)
BUILDING 6	220,244SF	102,500SF	2,500SF	106,000SF	45.8%	12.7%	118 STALLS (1,11/1000)
BUILDING 7	176,672SF	70,000SF	2,500SF	72,500SF	41.3%	18.8%	86 STALLS (1,31/1000)
TOTAL	2,181,114SF	927,500SF	17,500SF	948,000SF	43.3%	12.8%	1,000 STALLS (1,11/1000)

- KEY NOTES:**
- ① ASPHALT DRIVE
 - ② CONCRETE CURB
 - ③ LANDSCAPE AREA
 - ④ PAVING (SEE LANDSCAPE)
 - ⑤ STANDARD PARKING STALL
 - ⑥ ACCESSIBLE PARKING
 - ⑦ 8' HEIGHT CONC. SCREEN WALL
 - ⑧ ROLLING GATE
 - ⑨ CONC. TRUCK DOCKING AREA
 - ⑩ TRASH ENCLOSURE
 - ⑪ 8' HEIGHT TUBULAR STL. FENCE
 - ⑫ 42" GUARD RAIL
 - ⑬ 6' HEIGHT TUBULAR STL. FENCE



the ranch at Eastvale
Building 2 Site Plan
Eastvale, California

CLIENT: SUMMIT DEVELOPMENT CORPORATION
DRAWING DATE: 06/06/2016
Eastvale Planning Project No.: 15-0783
A2.0

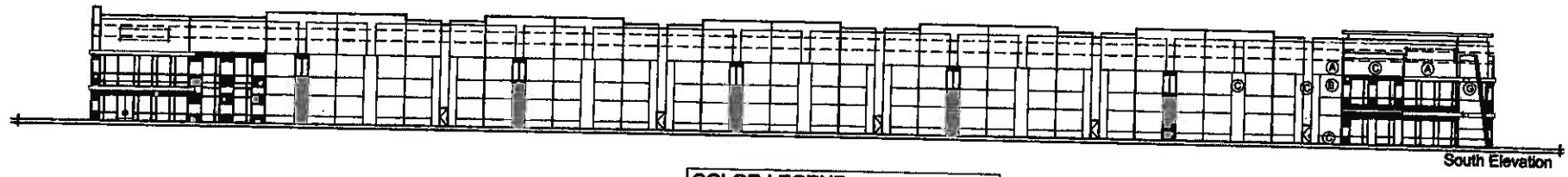
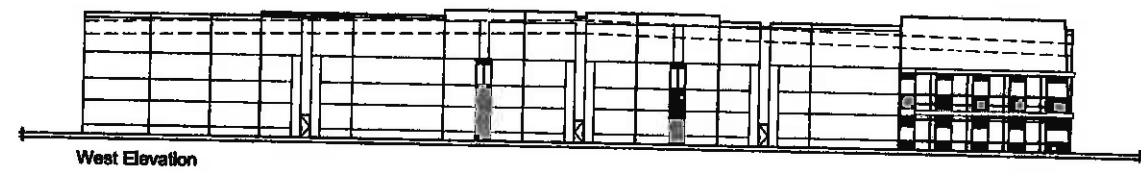
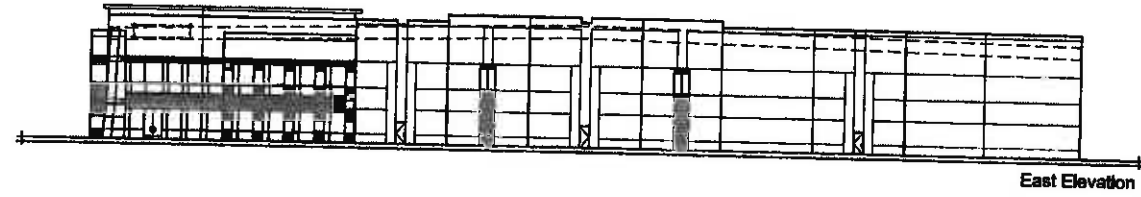
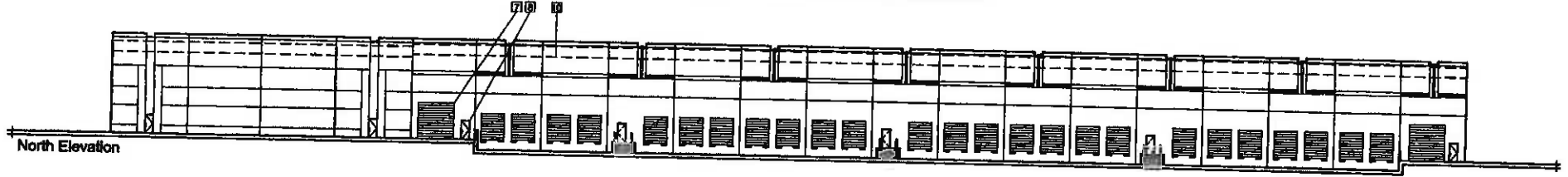


the ranch at Eastvale
 Building 2 Floor Plan
 Eastvale, California

CLIENT:
SUMMIT DEVELOPMENT CORPORATION
 DRAWING DATE: 05/06/2015
 Eastvale Planning Project No.: 15-0783

COLOR LEGEND:

Ⓐ VISTA LIGHT CREAM 8764	Ⓔ VISTA SAGE 8309
Ⓑ VISTA MEDIUM CREAM 8771	Ⓕ VISTA RED CLAY 8615
Ⓒ VISTA BIEGE 8585	Ⓖ VISTA TOUPE 8768



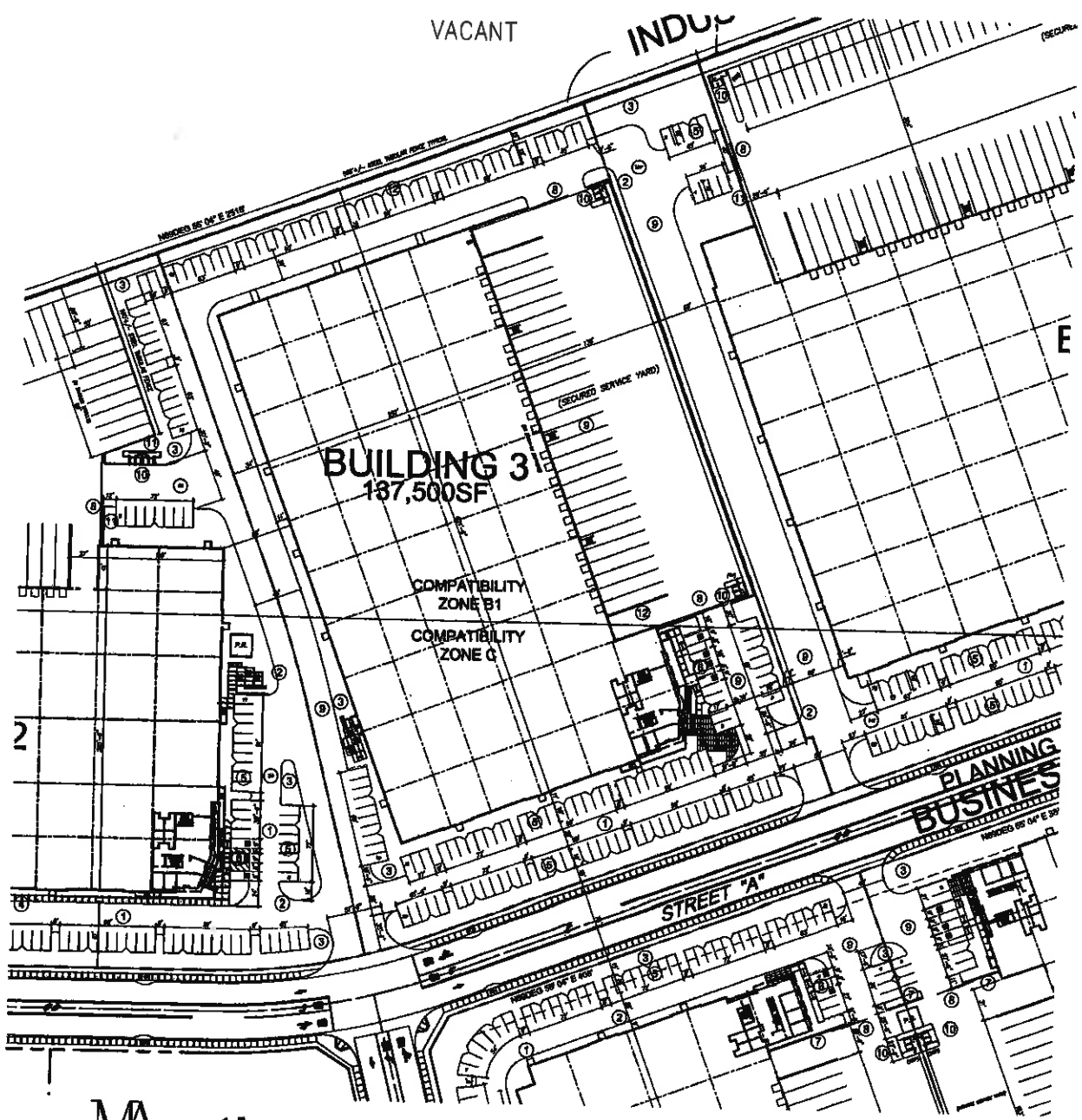
COLOR LEGEND:

Ⓐ VISTA LIGHT CREAM 8764	Ⓔ VISTA SAGE 8309
Ⓑ VISTA MEDIUM CREAM 8771	Ⓕ VISTA RED CLAY 8615
Ⓒ VISTA BIEGE 8585	Ⓖ VISTA TOUPE 8768



the ranch at Eastvale
 Building 2 Elevations
 Eastvale, California

CLIENT: **SUMMIT DEVELOPMENT CORPORATION**
 DRAWING DATE: 05/08/2015
 Eastvale Planning Project No.: 15-0783



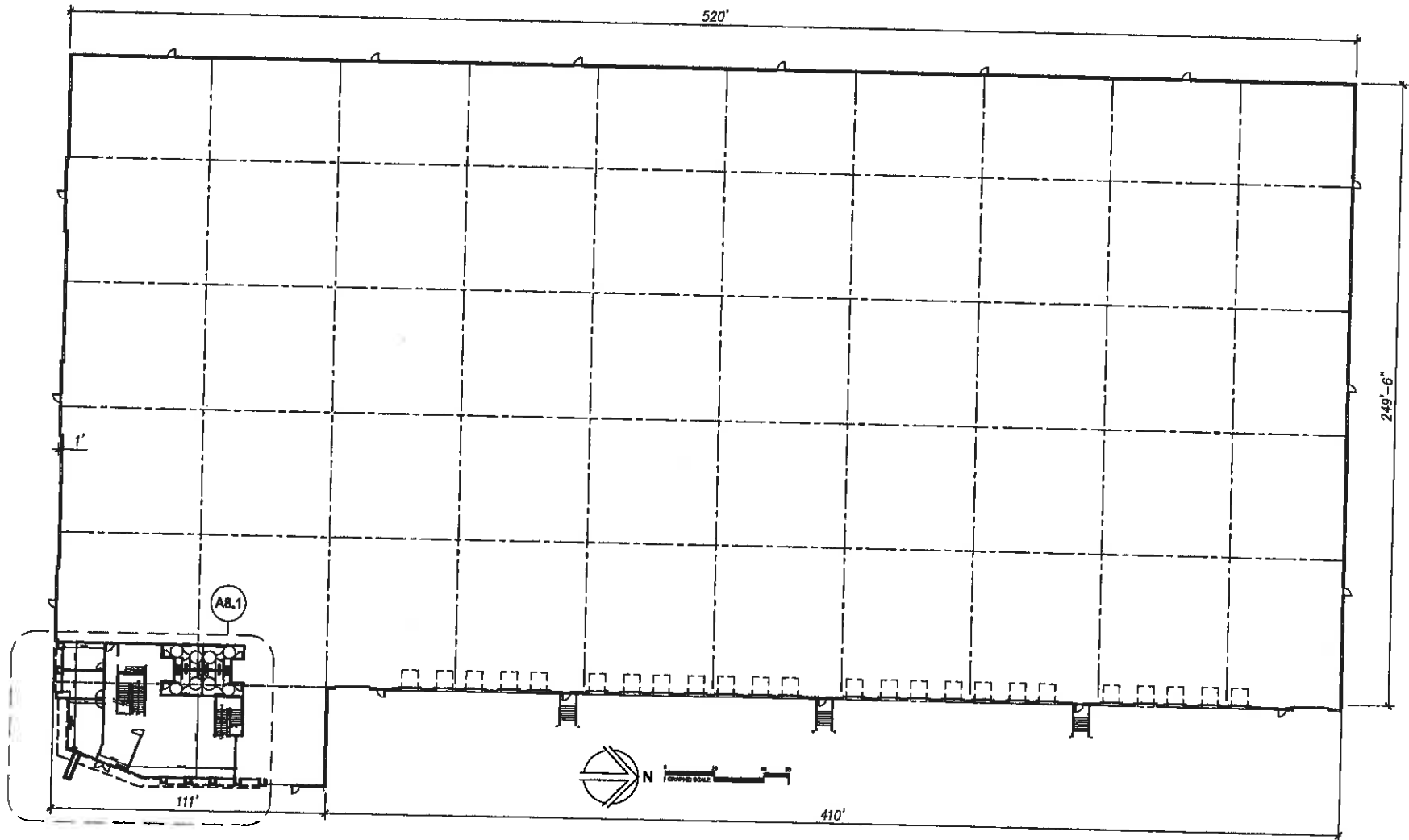
SITE DATA TABLE							
BUILDING	LAND AREA	1ST FLR (SQ.FT.)	MEZZ (SQ.FT.)	BUILDING (SQ.FT.)	F.A.R. %	LANDSCAP %	PARKING STALLS
BUILDING 1	145,720SF	47,000SF	2,800SF	50,000SF	34.8%	10.0%	82 STALLS (1.26/1000)
BUILDING 2	389,860SF	154,000SF	2,800SF	158,000SF	40.7%	10.0%	148 STALLS (1.53/1000)
BUILDING 3	238,282SF	135,000SF	2,800SF	137,000SF	48.4%	11.7%	138 STALLS (1/1000)
BUILDING 4	577,384SF	287,500SF	2,800SF	300,000SF	44.2%	11.5%	318 STALLS (1.09/1000)
BUILDING 5	281,261SF	121,000SF	2,800SF	123,000SF	43.2%	15.8%	172 STALLS (1.4/1000)
BUILDING 6	228,248SF	102,500SF	2,800SF	105,000SF	45.8%	12.7%	118 STALLS (1.17/1000)
BUILDING 7	175,873SF	70,000SF	2,800SF	72,000SF	41.3%	18.8%	98 STALLS (1.31/1000)
TOTAL	2,181,114SF	927,000SF	17,000SF	945,000SF	43.3%	12.5%	1,090 STALLS (1.51/1000)

- KEY NOTES:**
- ① ASPHALT DRIVE
 - ② CONCRETE CURB
 - ③ LANDSCAPE AREA
 - ④ PAVING (SEE LANDSCAPE)
 - ⑤ STANDARD PARKING STALL
 - ⑥ ACCESSIBLE PARKING
 - ⑦ 8' HEIGHT CONC. SCREEN WALL
 - ⑧ ROLLING GATE
 - ⑨ CONC. TRUCK DOCKING AREA
 - ⑩ TRASH ENCLOSURE
 - ⑪ 8' HEIGHT TUBULAR STL. FENCE
 - ⑫ 42" GUARD RAIL
 - ⑬ 6' HEIGHT TUBULAR STL. FENCE

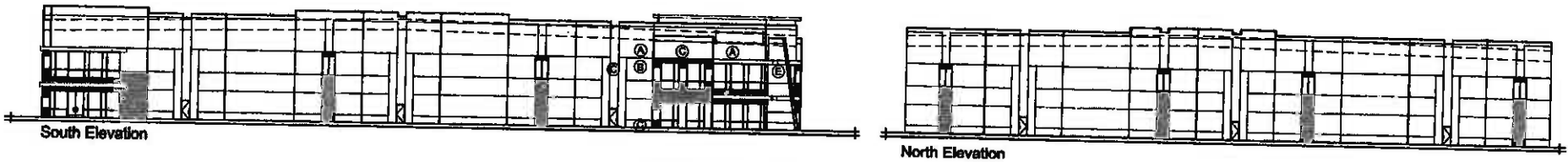
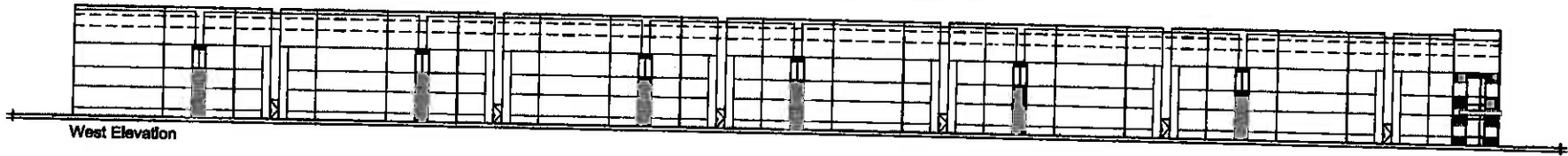


the ranch at Eastvale
 Building 3 Site Plan
 Eastvale, California

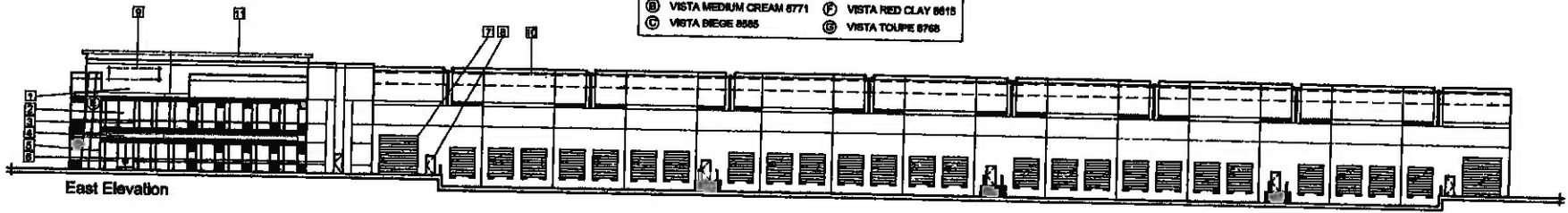
CLIENT:
SUMMIT DEVELOPMENT CORPORATION
 DRAWING DATE: 05/06/2015
 Eastvale Planning Project No.: 15-0783

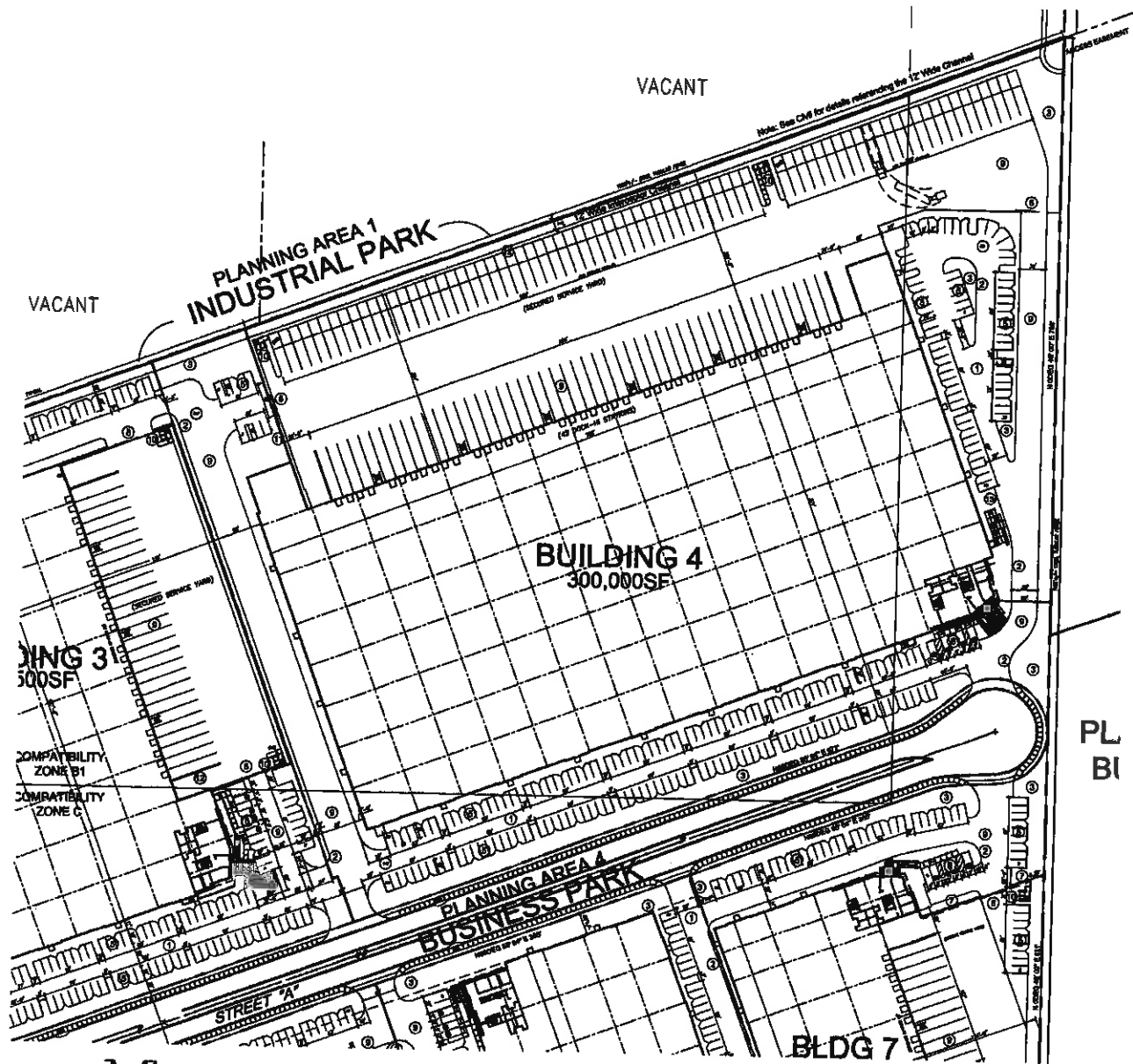


KEY NOTES:			
1	CONCRETE TILT UP PANEL	7	GRADE DOOR
2	MEDIUM PERFORMANCE GLASS	8	MAN DOOR
3	CLEAR ANODIZED ALUMINUM STOREFRONT	9	TENANT SIGNAGE
4	CLEAR ANODIZED ALUMINUM CANOPY	10	LINE OF ROOF BEYOND
5	CONCRETE FORMLINER	11	ALUMINUM CLAD CORNICE
6	2" WIDE REVEAL X 1-1/2" DEEP	12	N/A
		13	N/A



COLOR LEGEND:			
A	VISTA LIGHT CREAM 8784	E	VISTA SAGE 8360
B	VISTA MEDIUM CREAM 8771	F	VISTA RED CLAY 8618
C	VISTA DIEGE 8085	G	VISTA TOURPE 8768





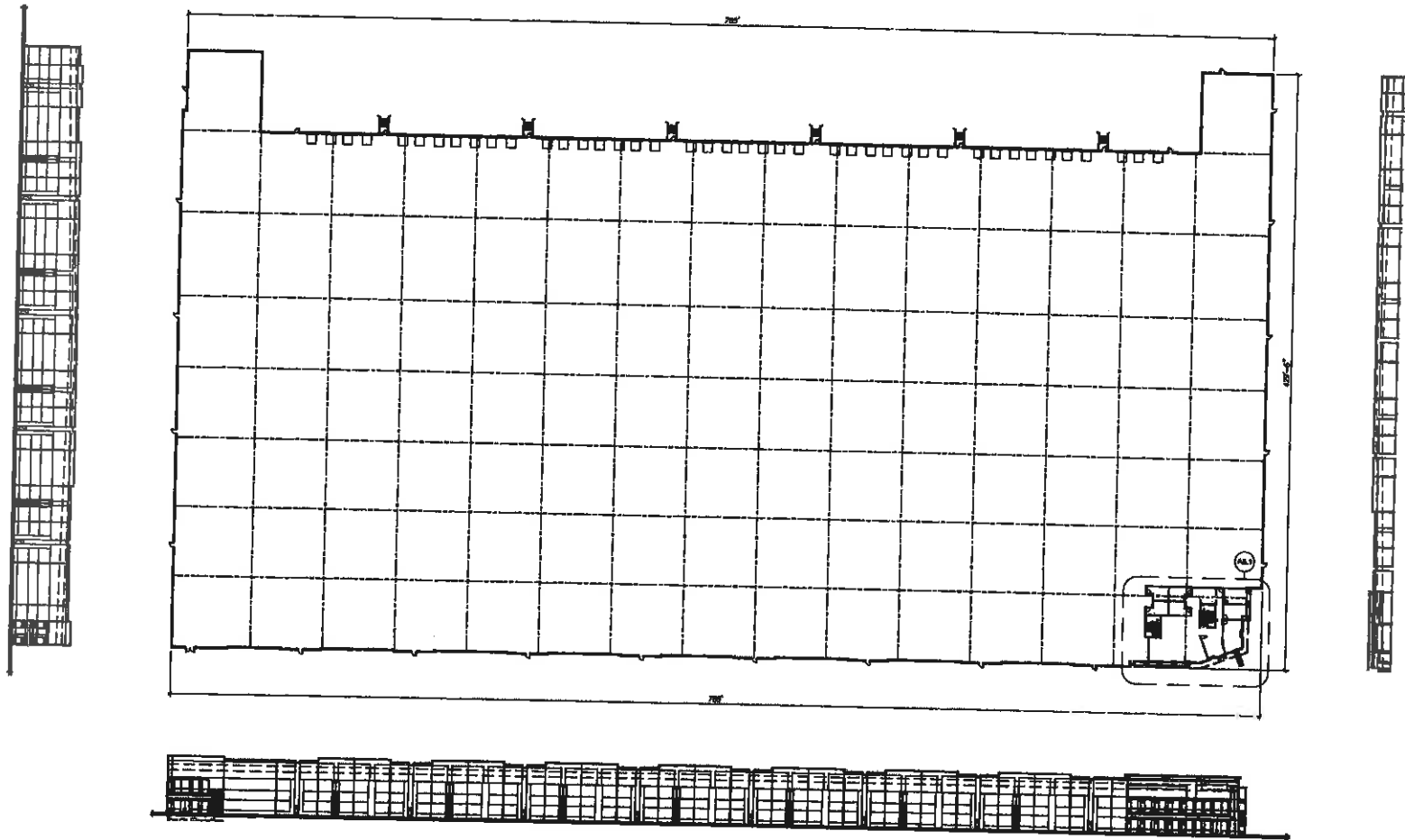
SITE DATA TABLE							
BUILDING	LAND AREA	1ST FTL (SF)	MEZZ. (SF)	BUILDING (SF)	F.A.R. %	LANDSCP %	PARKING STALLS
BUILDING 1	143,720SF	47,000SF	2,500SF	50,000SF	34.8%	10.0%	89 STALLS (1,261/1000)
BUILDING 2	368,800SF	184,000SF	2,500SF	186,800SF	42.7%	10.0%	146 STALLS (1,001/1000)
BUILDING 3	260,252SF	135,000SF	2,500SF	137,500SF	48.4%	11.7%	138 STALLS (1,170/1000)
BUILDING 4	877,394SF	297,500SF	2,500SF	300,000SF	44.2%	11.0%	318 STALLS (1,051/1000)
BUILDING 5	291,861SF	121,000SF	2,500SF	123,500SF	42.3%	16.8%	172 STALLS (1,141/1000)
BUILDING 6	229,248SF	102,500SF	2,500SF	105,000SF	45.8%	12.7%	119 STALLS (1,110/1000)
BUILDING 7	178,873SF	70,000SF	2,800SF	72,800SF	41.2%	18.0%	90 STALLS (1,211/1000)
TOTAL	2,181,114SF	927,500SF	17,800SF	945,300SF	43.2%	12.6%	1,000 STALLS (1,111/1000)

- KEY NOTES:**
- ① ASPHALT DRIVE
 - ② CONCRETE CURB
 - ③ LANDSCAPE AREA
 - ④ PAVING (SEE LANDSCAPE)
 - ⑤ STANDARD PARKING STALL
 - ⑥ ACCESSIBLE PARKING
 - ⑦ 9' HEIGHT CONC. SCREEN WALL
 - ⑧ ROLLING GATE
 - ⑨ CONC. TRUCK DOCKING AREA
 - ⑩ TRASH ENCLOSURE
 - ⑪ 8' HEIGHT TUBULAR STL. FENCE
 - ⑫ 42" GUARD RAIL
 - ⑬ 6' HEIGHT TUBULAR STL. FENCE



the ranch at Eastvale
 Building 4 Site Plan
 Eastvale, California

CLIENT: SUMMIT DEVELOPMENT CORPORATION
 DRAWING DATE: 06/08/2015
 Eastvale Planning Project No.: 15-0783



COLOR LEGEND:

1. WHITE BRICK CLAD WALL	2. WHITE BRICK CLAD ROOF
3. WHITE BRICK CLAD CORNER	4. WHITE BRICK CLAD GABLE
5. WHITE BRICK CLAD	6. WHITE BRICK CLAD



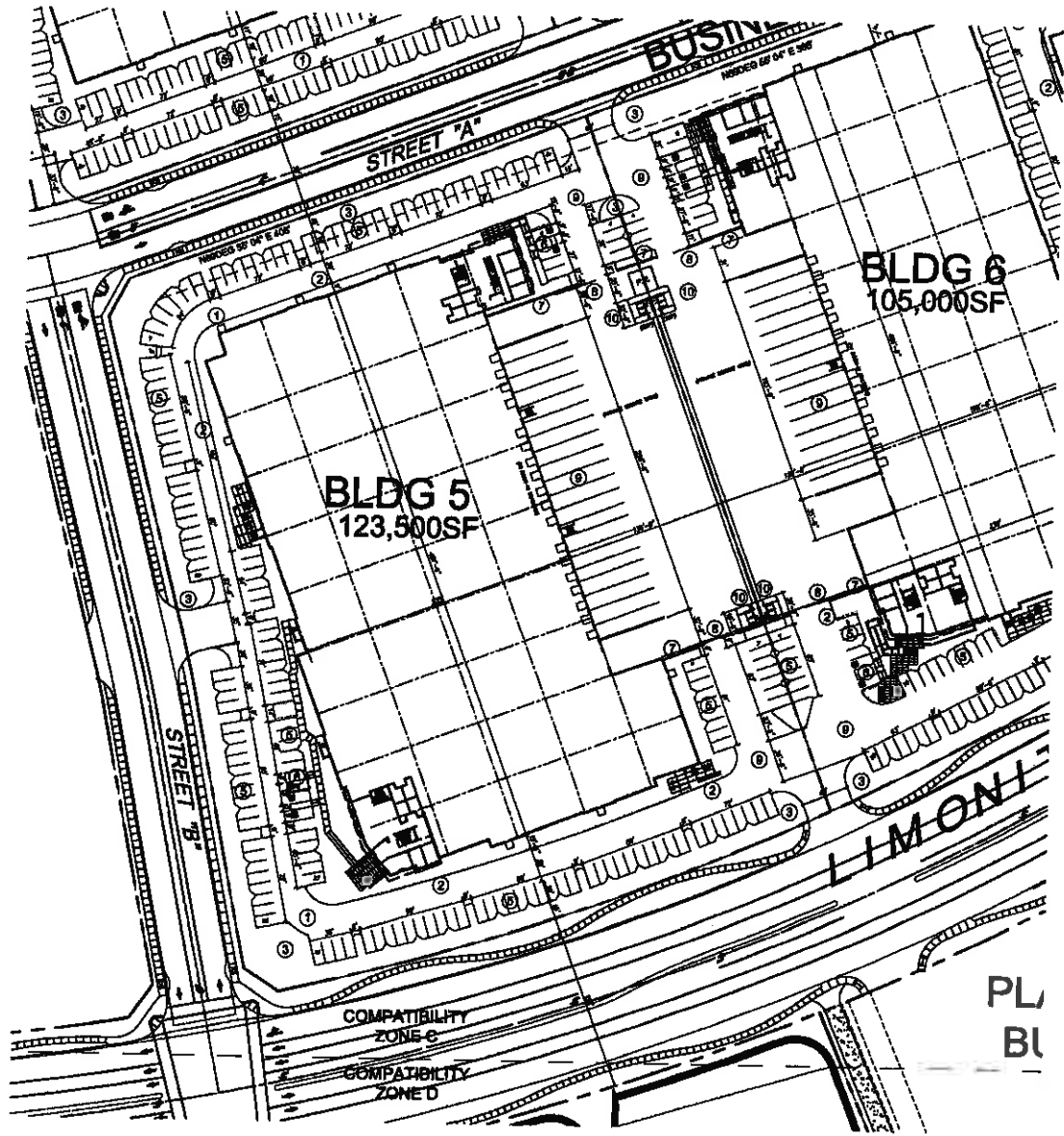
KEY NOTES:

1. CONCRETE TYP. CP. WALL	7. BRICKWORK
2. BRICK FORMWORK CLAD	8. METAL ROOF
3. CLAD. FINISHED ALUMINUM CURTAIN WALL	9. FINISHED BRICK
4. CONCRETE FINISH ROOF	10. LINE OF FINISH FLOOR
5. CONCRETE FINISH ROOF	11. FINISHED CLAD ROOF
6. 2" BRICK ROOF & 1" MET. ROOF	12. FIN.



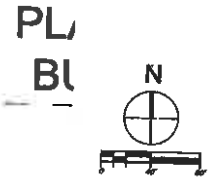
the ranch at Eastvale
 Building 4 Elevations
 Eastvale, California

CLIENT:
SUMMIT DEVELOPMENT CORPORATION
 DRAWING DATE: 06/08/2015
 Eastvale Planning Project No.: 15-0783



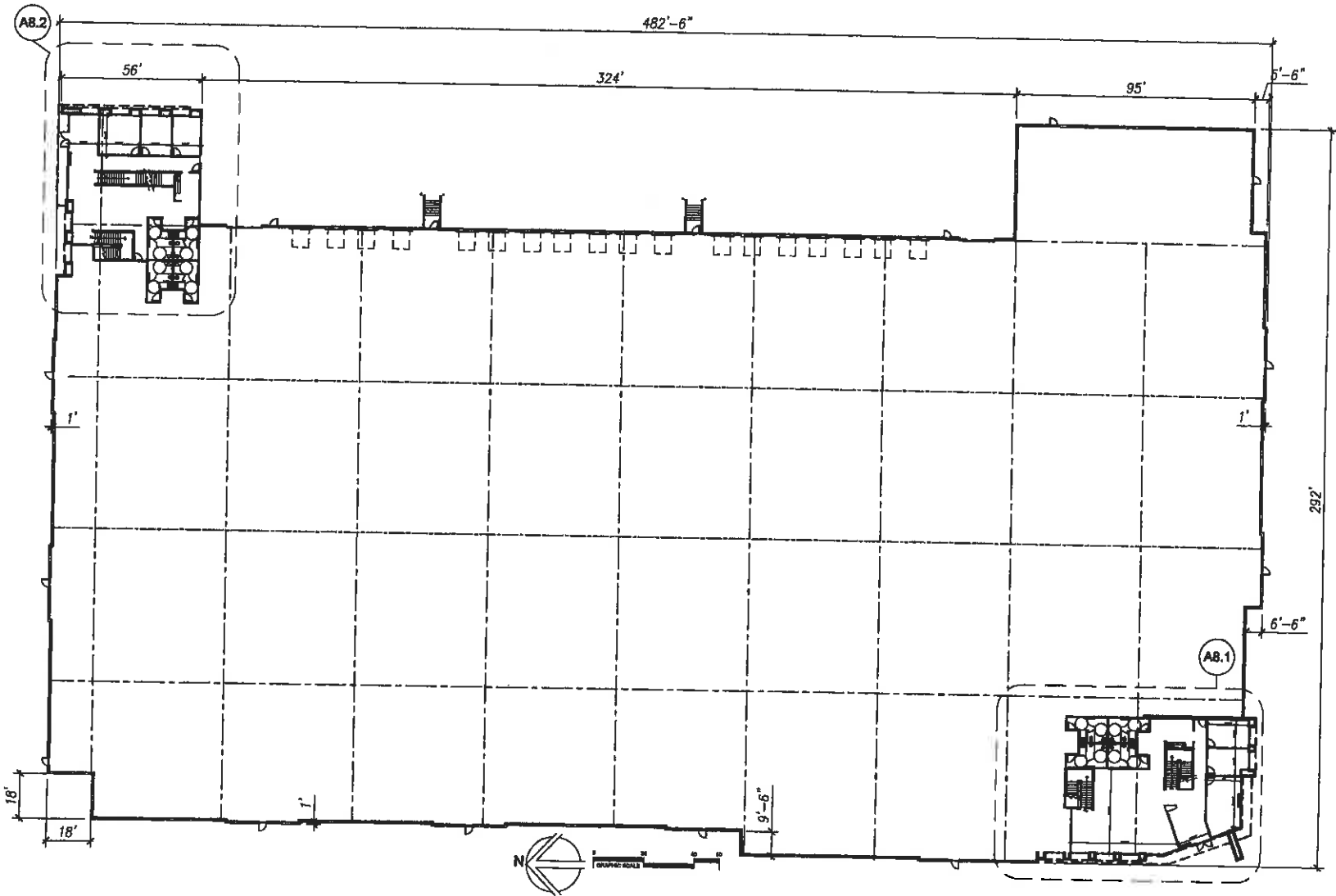
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BUILDING	LAND AREA	1ST FLR (SF)	MEZZ (SF)	BUILDING (SF)	F.A.R. %	LANDSCP %	PARKING STALLS
BUILDING 1	143,720SF	47,000SF	2,000SF	90,000SF	34.8%	10.0%	66 STALLS (1.26/1000)
BUILDING 2	388,600SF	104,000SF	2,000SF	198,000SF	42.7%	10.0%	143 STALLS (1.83/1000)
BUILDING 3	298,762SF	136,000SF	2,000SF	137,000SF	46.4%	11.7%	138 STALLS (1.1/1000)
BUILDING 4	877,394SF	207,000SF	2,000SF	300,000SF	44.3%	11.9%	318 STALLS (1.05/1000)
BUILDING 5	291,361SF	121,000SF	2,000SF	123,000SF	42.3%	18.8%	172 STALLS (1.4/1000)
BUILDING 6	228,244SF	102,000SF	2,000SF	105,000SF	46.0%	12.7%	118 STALLS (1.1/1000)
BUILDING 7	175,678SF	70,000SF	2,000SF	72,000SF	41.3%	18.9%	96 STALLS (1.31/1000)
TOTAL	2,181,114SF	827,000SF	17,000SF	846,000SF	43.9%	12.6%	1,069 STALLS (1.17/1000)

- KEY NOTES:**
- ① ASPHALT DRIVE
 - ② CONCRETE CURB
 - ③ LANDSCAPE AREA
 - ④ PAVING (SEE LANDSCAPE)
 - ⑤ STANDARD PARKING STALL
 - ⑥ ACCESSIBLE PARKING
 - ⑦ 8' HEIGHT CONC. SCREEN WALL
 - ⑧ ROLLING GATE
 - ⑨ CONC. TRUCK DOCKING AREA
 - ⑩ TRASH ENCLOSURE
 - ⑪ 8' HEIGHT TUBULAR STL. FENCE
 - ⑫ 42" GUARD RAIL
 - ⑬ 6' HEIGHT TUBULAR STL. FENCE



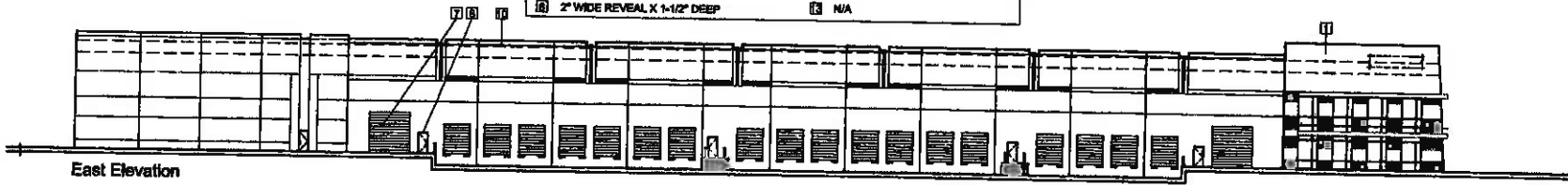
the ranch at Eastvale
 Building 5 Site Plan
 Eastvale, California

CLIENT:
SUMMIT DEVELOPMENT CORPORATION
 DRAWING DATE: 05/08/2015
 Eastvale Planning Project No.: 15-0783
 A5.0

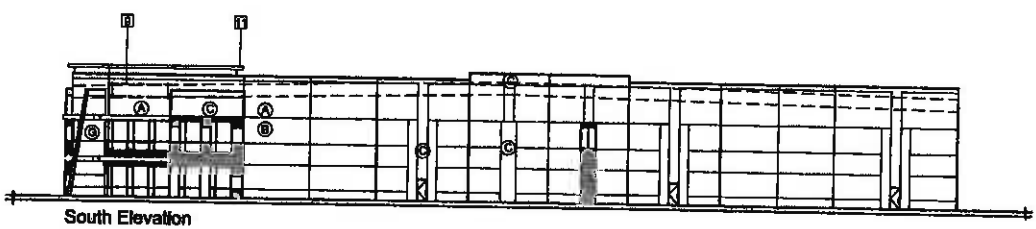


KEY NOTES:

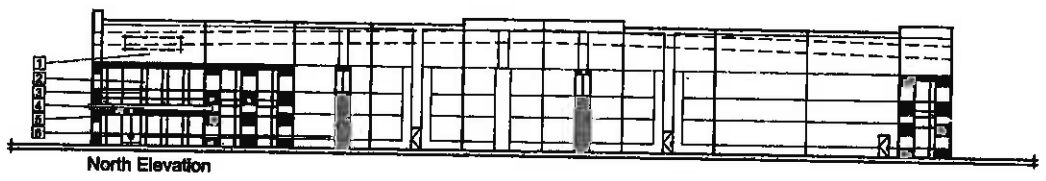
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2 MEDIUM PERFORMANCE GLASS	8 MAN DOOR
3 CLEAR ANODIZED ALUMINUM STOREFRONT	9 TENANT SIGNAGE
4 CLEAR ANODIZED ALUMINUM CANOPY	10 LINE OF ROOF BEYOND
5 CONCRETE FORMLINER	11 ALUMINUM CLAD CORNICE
6 2" WIDE REVEAL X 1-1/2" DEEP	12 N/A
	13 N/A



East Elevation



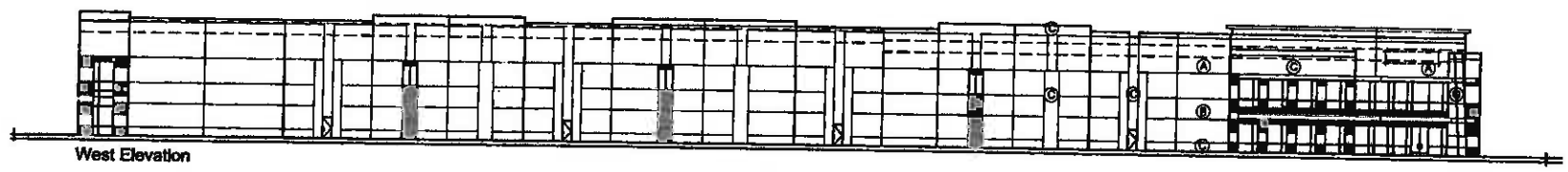
South Elevation



North Elevation

COLOR LEGEND:

A VISTA LIGHT CREAM 6764	E VISTA SAGE 6066
B VISTA MEDIUM CREAM 6771	F VISTA RED CLAY 6615
C VISTA BIEGE 6586	G VISTA TOULPE 6788

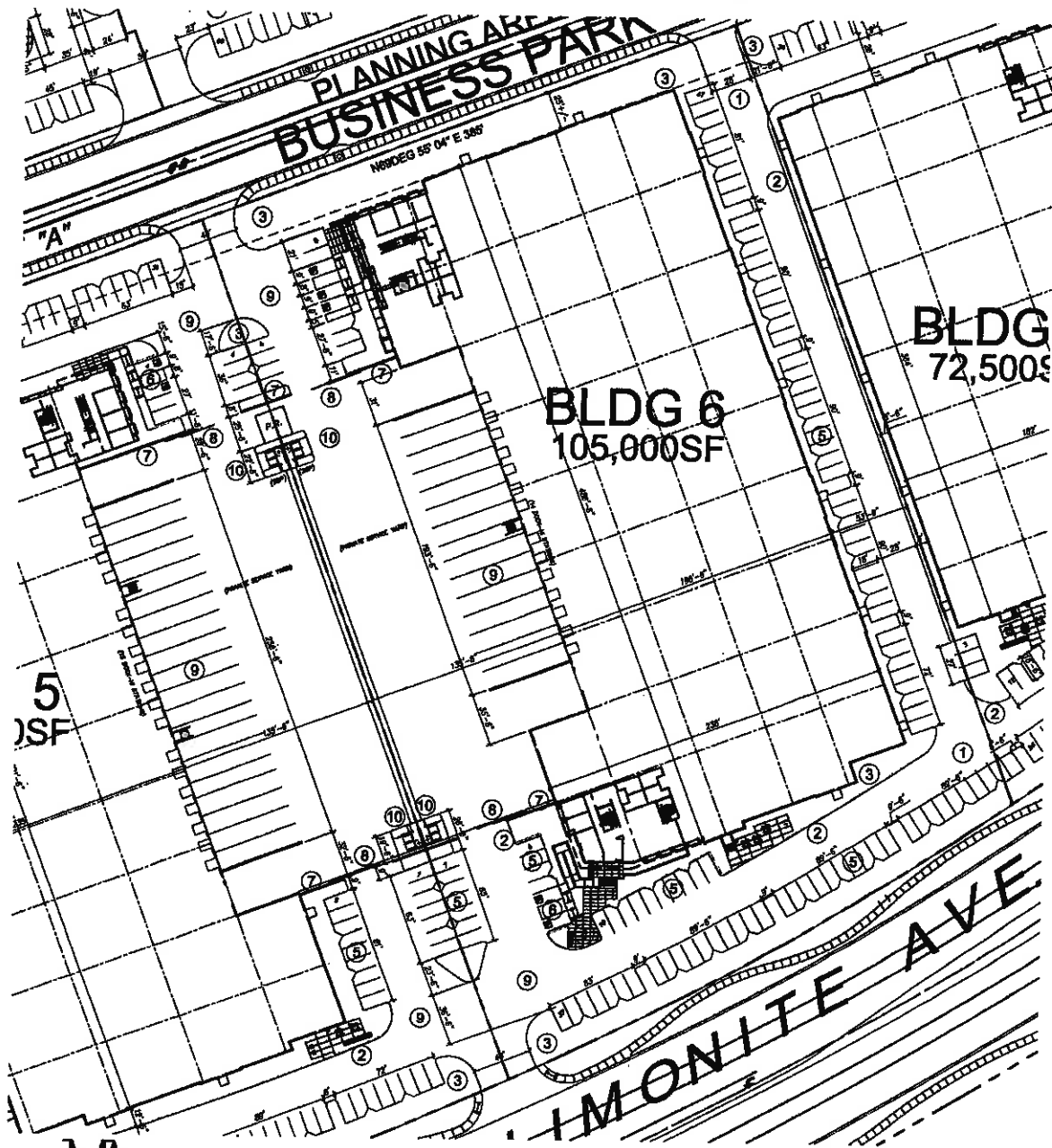


West Elevation



the ranch at Eastvale
 Building 5 Elevations
 Eastvale, California

CLIENT:
SUMMIT DEVELOPMENT CORPORATION
 DRAWING DATE: 05/08/2015
 Eastvale Planning Project No.: 15-0783



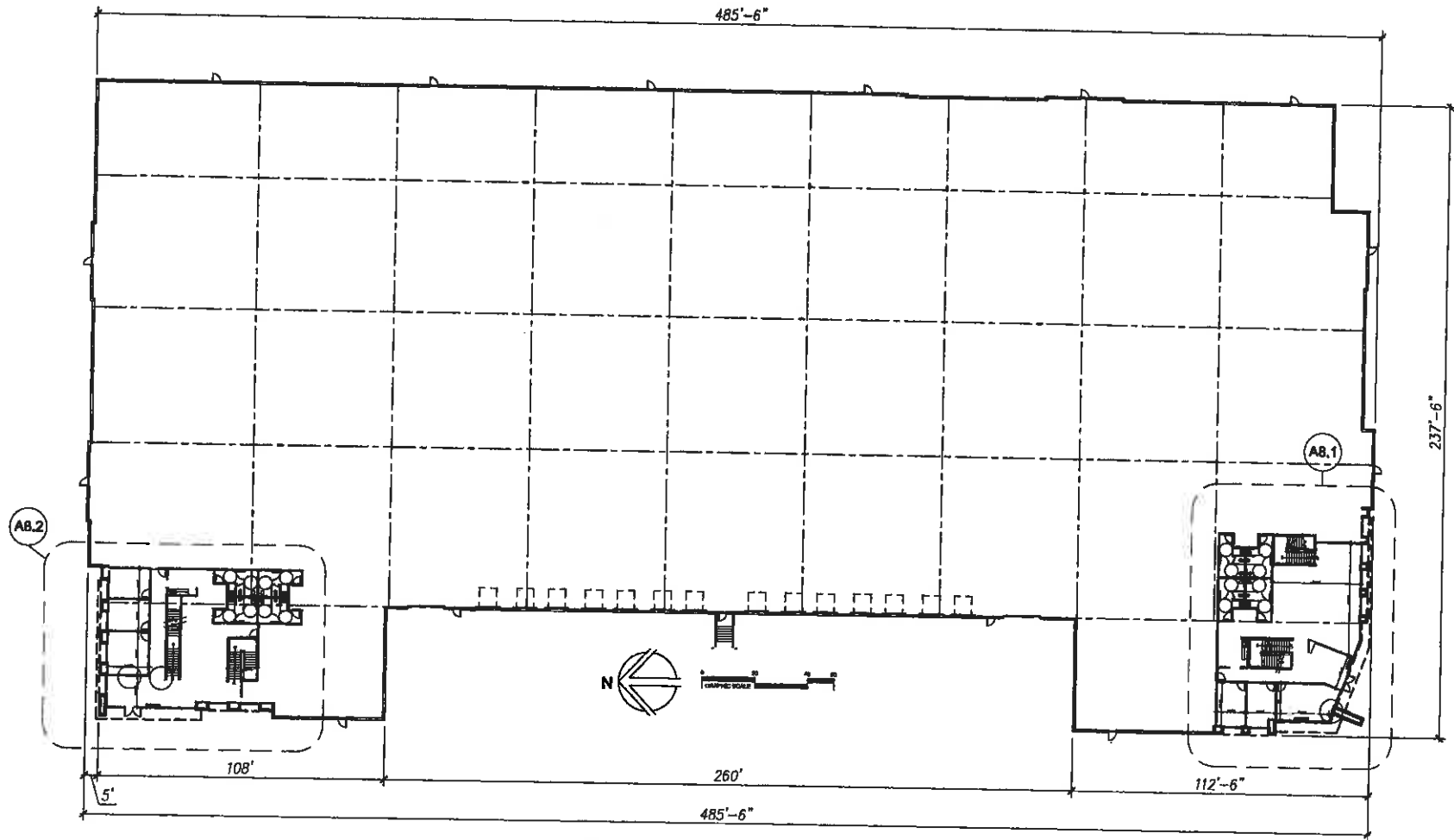
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BUILDING	LAND AREA	TOT. F.L.A. (S.F.)	MEZZ. (S.F.)	BUILDING (S.F.)	F.A.R. %	LANDSCP %	PARKING STALLS
BUILDING 1	143,720SF	47,800SF	2,000SF	80,000SF	34.0%	10.0%	88 STALLS (1.30/1000)
BUILDING 2	308,880SF	184,000SF	2,500SF	188,000SF	42.7%	18.0%	148 STALLS (.80/1000)
BUILDING 3	298,232SF	180,000SF	2,000SF	137,800SF	46.4%	11.7%	138 STALLS (1/1000)
BUILDING 4	677,384SF	287,500SF	2,800SF	300,000SF	44.2%	11.6%	316 STALLS (1.00/1000)
BUILDING 5	281,001SF	121,000SF	2,500SF	123,000SF	43.3%	16.8%	172 STALLS (1.41/1000)
BUILDING 6	229,248SF	102,800SF	2,500SF	108,000SF	48.0%	12.7%	118 STALLS (1.41/1000)
BUILDING 7	175,873SF	70,000SF	2,500SF	72,000SF	41.3%	18.9%	88 STALLS (1.51/1000)
TOTAL	2,181,114SF	927,800SF	17,500SF	948,000SF	43.3%	12.8%	1,080 STALLS (1.11/1000)

- KEY NOTES:**
- ① ASPHALT DRIVE
 - ② CONCRETE CURB
 - ③ LANDSCAPE AREA
 - ④ PAVING (SEE LANDSCAPE)
 - ⑤ STANDARD PARKING STALL
 - ⑥ ACCESSIBLE PARKING
 - ⑦ 9' HEIGHT CONC. SCREEN WALL
 - ⑧ ROLLING GATE
 - ⑨ CONC. TRUCK DOCKING AREA
 - ⑩ TRASH ENCLOSURE
 - ⑪ 8' HEIGHT TUBULAR STL. FENCE
 - ⑫ 42" GUARD RAIL
 - ⑬ 6' HEIGHT TUBULAR STL. FENCE



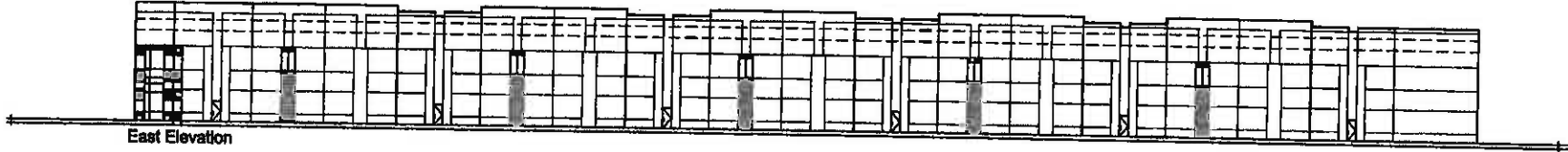
the ranch at Eastvale
 Building 6 Site Plan
 Eastvale, California

CLIENT:
SUMMIT DEVELOPMENT CORPORATION
 DRAWING DATE: 05/08/2015
 Eastvale Planning Project No.: 15-0783

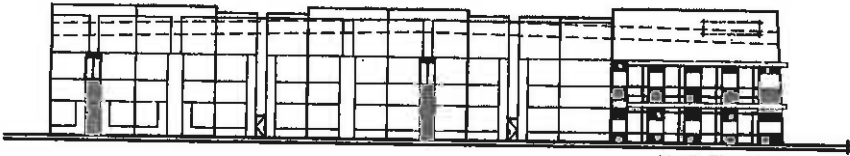


KEY NOTES:

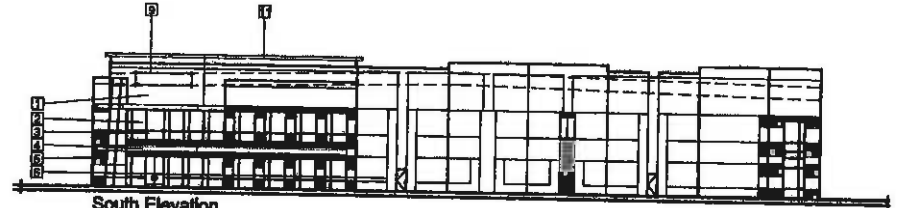
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2 MEDIUM PERFORMANCE GLASS	8 MAN DOOR
3 CLEAR ANODIZED ALUMINUM STOREFRONT	9 TENANT SIGNAGE
4 CLEAR ANODIZED ALUMINUM CANOPY	10 LINE OF ROOF BEYOND
5 CONCRETE FORMLINER	11 ALUMINUM CLAD CORNICE
6 2" WIDE REVEAL X 1-1/2" DEEP	12 N/A
	13 N/A



East Elevation



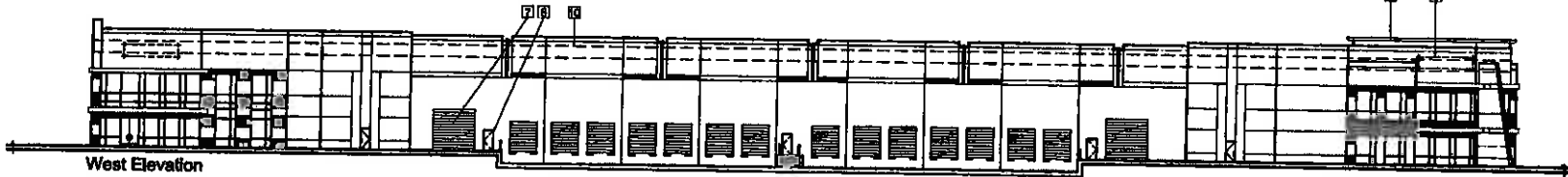
North Elevation



South Elevation

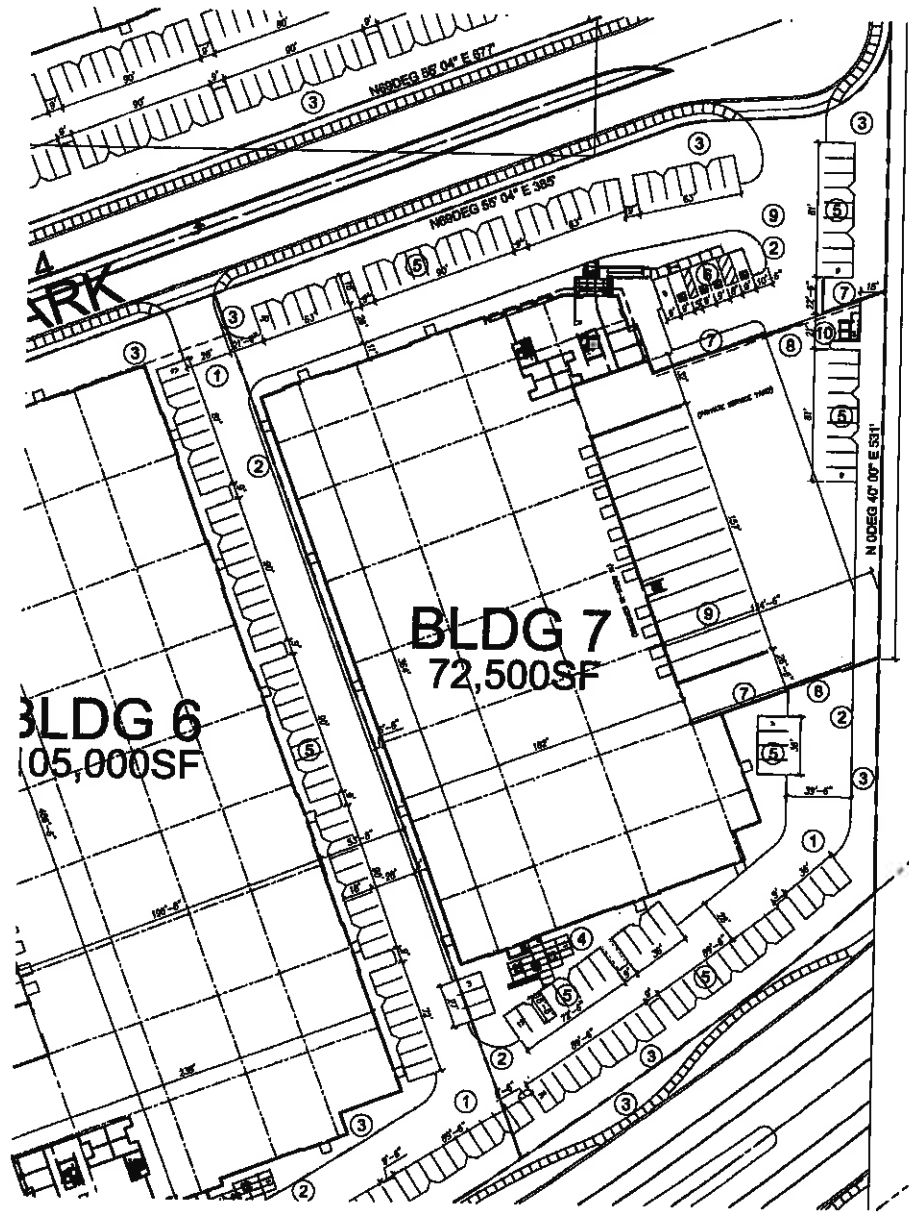
COLOR LEGEND:

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B VISTA MEDIUM CREAM 6771	F VISTA RED CLAY 8615
C VISTA BIEGE 8305	G VISTA TOUPE 8708



West Elevation





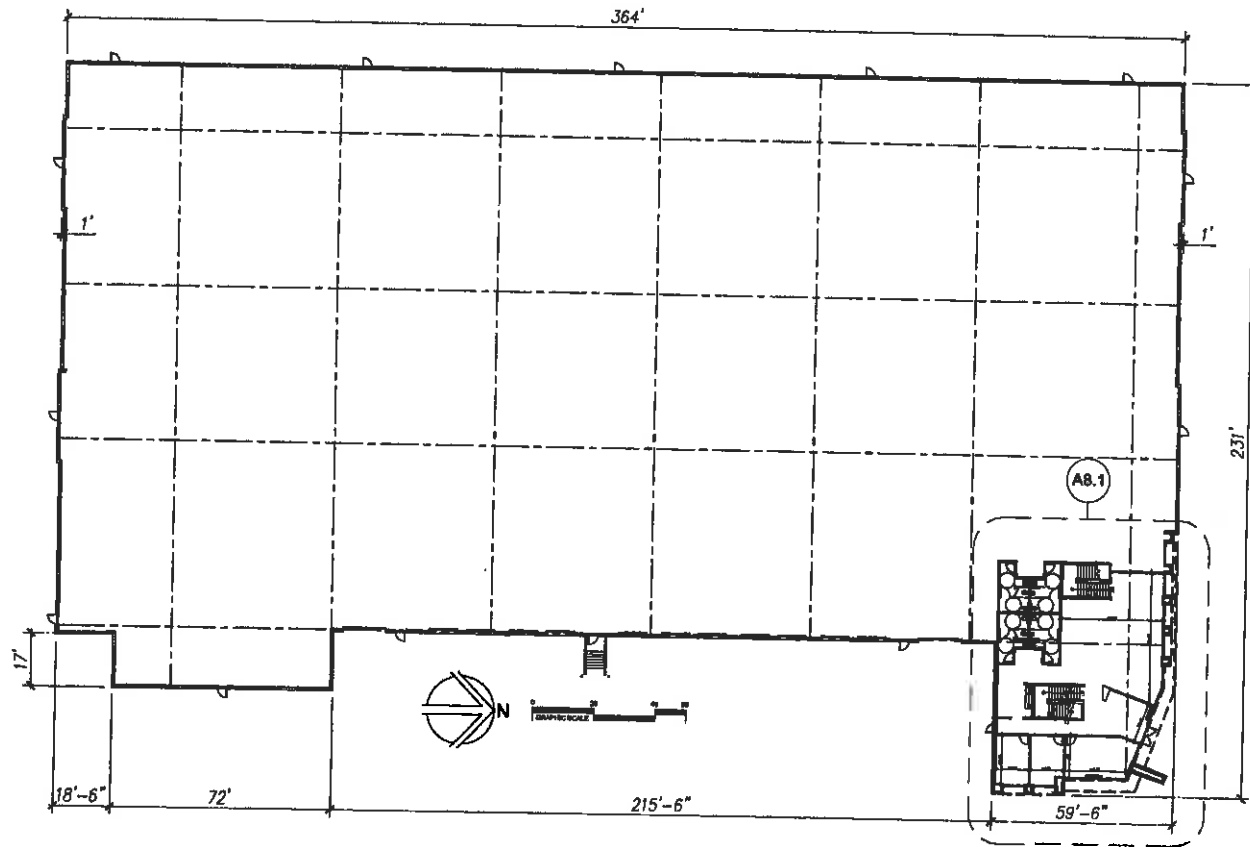
SITE DATA TABLE							
BUILDING	LAND AREA	1ST FLR. (R.F.)	2ND FLR. (R.F.)	BUILDING (R.F.)	F.A.R. %	LANDSCAP %	PARKING STALLS
BUILDING 1	143,720SF	47,800SF	2,800SF	80,000SF	34.8%	10.0%	88 STALLS (1.89/1000)
BUILDING 2	368,850SF	154,000SF	2,800SF	188,800SF	42.7%	10.0%	148 STALLS (8.0/1000)
BUILDING 3	298,280SF	136,000SF	2,800SF	137,200SF	48.4%	14.7%	138 STALLS (11/1000)
BUILDING 4	877,344SF	297,500SF	2,800SF	305,000SF	44.3%	11.6%	318 STALLS (1.06/1000)
BUILDING 5	281,851SF	121,000SF	2,800SF	123,800SF	42.3%	14.9%	172 STALLS (1.4/1000)
BUILDING 6	229,844SF	102,000SF	2,800SF	108,000SF	48.8%	12.7%	116 STALLS (1.1/1000)
BUILDING 7	175,879SF	70,000SF	2,800SF	72,800SF	41.2%	18.9%	88 STALLS (1.8/1000)
TOTAL	2,181,114SF	927,800SF	17,500SF	945,000SF	43.3%	12.6%	1,050 STALLS (1.1/1000)

- KEY NOTES:**
- ① ASPHALT DRIVE
 - ② CONCRETE CURB
 - ③ LANDSCAPE AREA
 - ④ PAVING (SEE LANDSCAPE)
 - ⑤ STANDARD PARKING STALL
 - ⑥ ACCESSIBLE PARKING
 - ⑦ 9' HEIGHT CONC. SCREEN WALL
 - ⑧ ROLLING GATE
 - ⑨ CONC. TRUCK DOCKING AREA
 - ⑩ TRASH ENCLOSURE
 - ⑪ 8' HEIGHT TUBULAR STL. FENCE
 - ⑫ 42" GUARD RAIL
 - ⑬ 6' HEIGHT TUBULAR STL. FENCE



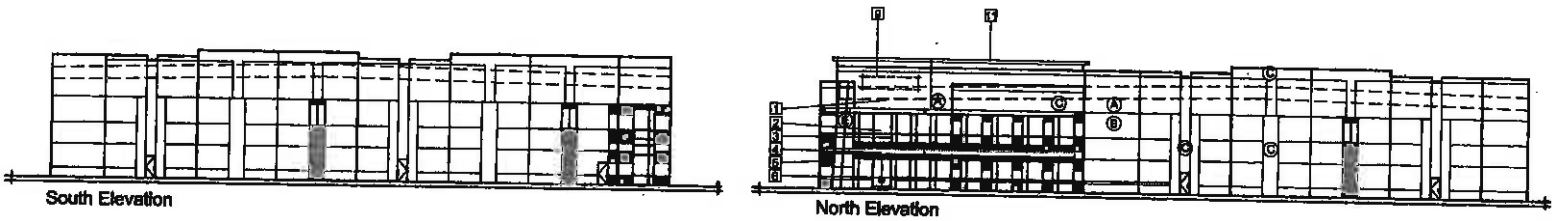
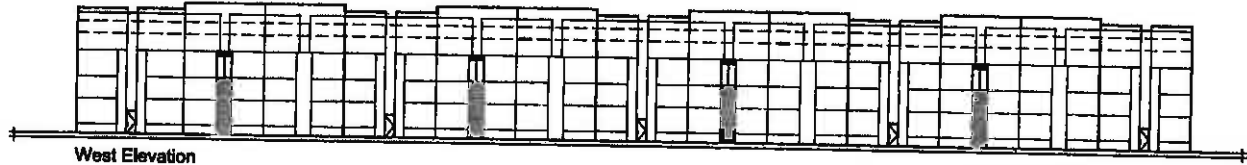
the ranch at Eastvale
 Building 7 Site Plan
 Eastvale, California

CLIENT:
SUMMIT DEVELOPMENT CORPORATION
 DRAWING DATE: 06/08/2015
 Eastvale Planning Project No.: 15-0783



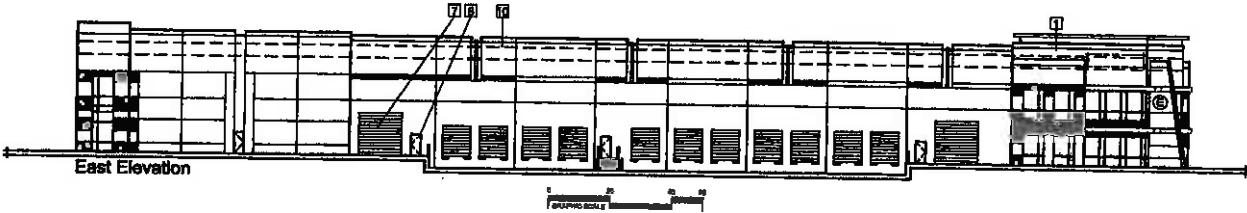
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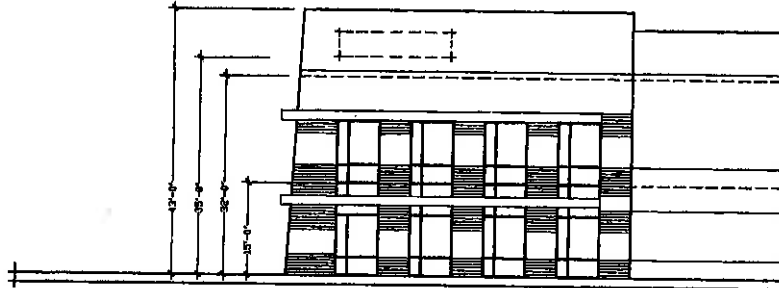
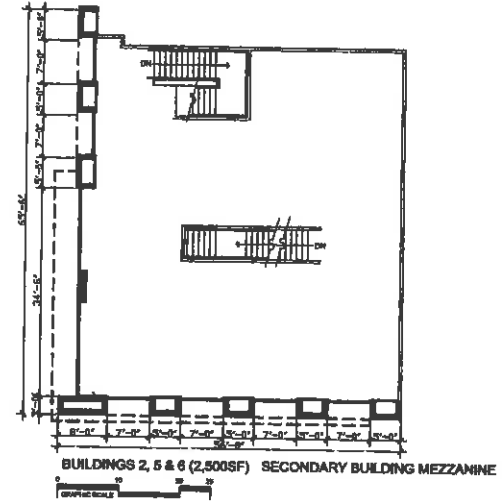
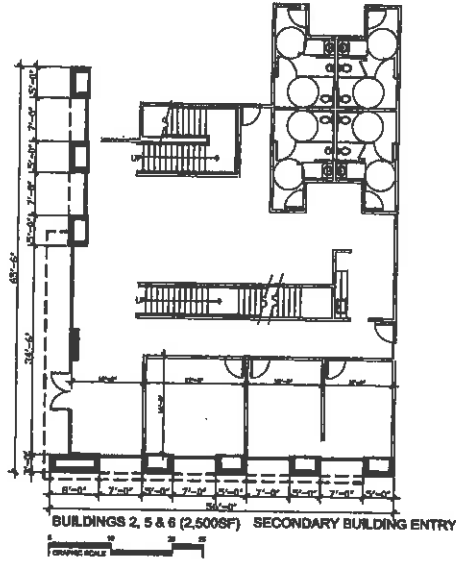
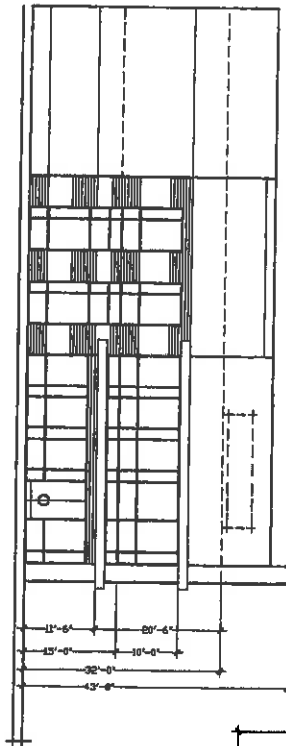
1 CONCRETE TILT UP PANEL	7 GRADE DOOR
2 MEDIUM PERFORMANCE GLASS	8 MAN DOOR
3 CLEAR ANODIZED ALUMINUM STOREFRONT	9 TENANT SIGNAGE
4 CLEAR ANODIZED ALUMINUM CANOPY	10 LINE OF ROOF BEYOND
5 CONCRETE FORMLINER	11 ALUMINUM GLAD CORNICE
6 2" WIDE REVEAL X 1-1/2" DEEP	12 N/A
	13 N/A



COLOR LEGEND:

A VISTA LIGHT CREAM 8764	E VISTA SAGE 6399
B VISTA MEDIUM CREAM 8771	F VISTA RED CLAY 8815
C VISTA BEIGE 8585	G VISTA TOULPE 8768







the
ranch
AT EASTVALE
Specific Plan

Project Applicant:

Summit Development Corporation
450 Newport Center Drive, Suite 625
Newport Beach, California 92660
Contact Person: Bryan Bentrott, Principal
(949) 655-8226

Lead Agency:

City of Eastvale
City of Eastvale Planning Department
12363 Limonite Avenue, Suite 910
Eastvale, CA 91752
Contact Person: Cathy Perring, Assistant Planning Director
(951) 361-0900

Prepared by:

Albert A. Webb Associates
3788 McCray Street
Riverside, CA 92506
Contact Person: Melissa Perez, Senior Environmental Planner
(951) 686-1070

Revised Screecheck No. 1 (ALUC): June 17, 2015

2.0 DEVELOPMENT STANDARDS

The Ranch at Eastvale Specific Plan provides the City of Eastvale, along with developers, community groups and community service districts, with a comprehensive set of plans, regulations, conditions and programs for guiding the systematic development of the project, and implements each applicable element of the City of Eastvale General Plan. The proposed The Ranch at Eastvale Specific Plan establishes the land use plan, designated planning areas, development standards and design and landscaping guidelines that will guide future development of the project area.

For purposes of The Ranch at Eastvale Specific Plan, development standards will be interpreted as required development features of the project and development guidelines will be interpreted as recommendations. Variations from guidelines in implementing projects will be administratively approved by the Planning Department and an amendment to the Specific Plan or a minor revision shall not be required. Variations from development standards in implementing projects will require, as determined in accordance with Section 2.5, either an amendment to the Specific Plan or a minor revision application.

2.1 Land Use Plan






The Ranch at Eastvale Specific Plan is designed to create a southern California mix of retail, office, and light industrial uses that will appeal to current and future residents and future business owners to meet a variety of needs. Around the perimeter of this business center, the historic heritage of the area will be celebrated through the re-introduction of plantings along the adjacent streets of Limonite Avenue and Hellman Avenue. The main entry into the business center will come from Limonite Avenue.

The proposed 119.9-acre The Ranch at Eastvale Specific Plan has been divided into nine Planning Areas with four different land uses as shown on **Figure 2-1, Land Use Plan** and **Table 2-1, Land Use Summary**. There is one commercial-retail Planning Area, totaling 5.9 acres; five business park Planning Areas, totaling 45.6 acres; two light industrial Planning Areas, totaling 44.5 acres, and one mixed-use Planning Area, totaling 11.6 acres; each reflecting elements of the architectural theme drawn from the agricultural heritage of the area.

Table 2-1, Land Use Summary

Land Use	Planning Area	Acreage
Commercial-Retail	5	5.9
	Subtotal	5.9
Business Park	3	5.6
	4	17.7
	6	10.5
	8	5.2
	9	6.6
	Subtotal	45.6
Light Industrial	1	36.1
	7	8.4
	Subtotal	44.5
Mixed Use	2	11.6
	Subtotal	11.6
Major Roads		12.3
	Subtotal	12.3
TOTAL ACREAGE		119.9

LEGEND

P.A.	LAND USE	GROSS AREA (A.C)
	COMMERCIAL RETAIL	5.9 AC
	LIGHT INDUSTRIAL	44.5 AC
	BUSINESS PARK	45.6 AC
	MIXED USE	11.6 AC
	RIGHT OF WAY	12.3 AC
TOTAL		119.9 AC

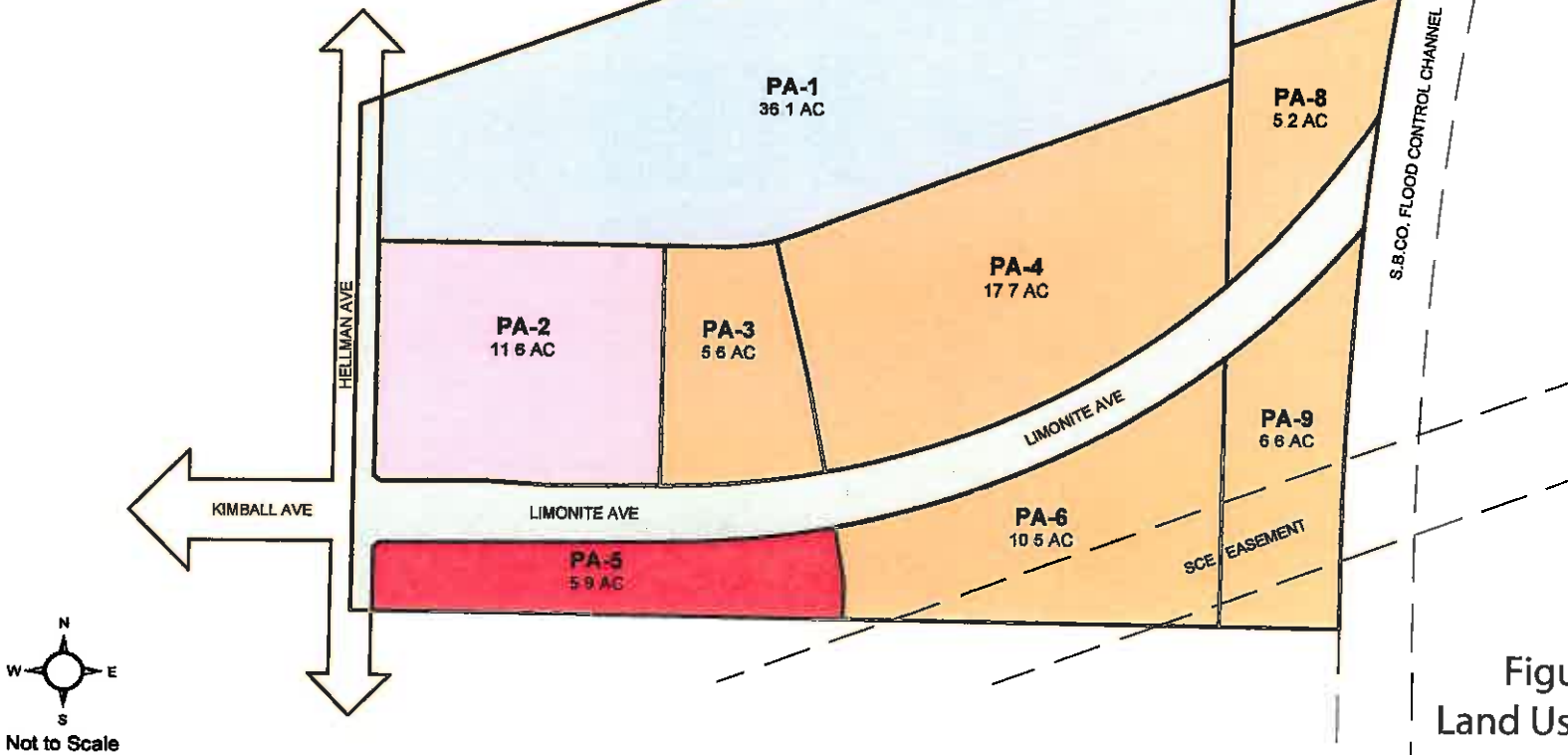


Figure 2-1
Land Use Plan

Allowable Land Uses

The Ranch at Eastvale Specific Plan is designed to provide a wide range of allowable land uses as identified in **Table 2-2, Allowable Land Uses** below, to respond to the market at the time development occurs.

Commercial-Retail

Commercial-retail land uses are intended to provide neighborhood and community commercial uses to serve current residents and future residents of the outlying area and the businesses within The Ranch at Eastvale Specific Plan. Allowable uses within the Commercial-Retail designation include those uses derived from commercial uses found in the City of Eastvale Municipal Zoning Code (EMC) and set forth in Table 2-2, below. Uses include, but are not limited to restaurants, shops, drug stores, markets, gasoline sales, hotel or motel, other commercial uses that would serve the needs of the local residents, public uses, private businesses and office uses. Each Commercial-Retail Planning Area's building intensity will not exceed a 0.35 Floor Area Ratio (FAR)¹.

Light Industrial

Light industrial land uses are proposed through the implementation of the Light Industrial designation. The purpose of this designation is to allow for a wide variety of light industrial and related uses, including, but not limited to, assembly, repair and light manufacturing, and limited retail and service facilities, that provide employment opportunities for local residents. Each Planning Area's building intensity will not exceed a 0.60 FAR. Land uses allowable within the Light Industrial designation are set forth in Table 2-2, below.

Business Park

Office, service commercial and light industrial uses are proposed through the implementation of the Business Park designation. The purpose of this designation is to allow for a wide variety of business park, light industrial, and related uses, as set forth in Table 2-2, below including, but not limited to research and development, technology centers, corporate headquarters and administrative and support offices in a campus-type setting. Each Planning Area's building intensity will not exceed a 0.60 FAR.

Mixed-Use

Mixed-Use land uses are intended to provide a mix of both commercial-retail and business park uses to serve both residents and small businesses. Allowable uses within the Mixed-Use designation include those uses derived from both commercial and business park uses found in the City of Eastvale Municipal Zoning Code (EMC) and set forth in Table 2-2, below. Uses include, but are not limited to restaurants, shops, drug stores, markets, gasoline sales, hotel or motel, research and development, technology centers, corporate headquarters and administrative and support offices. The Mixed-Use Planning Area building intensity will not exceed a 0.35 FAR.

¹ Floor Area Ratio is the gross building area of all floors divided by the lot area.

Table 2-2, Allowable Land Uses

Land Uses	Commercial-Retail	Mixed Use	Business Park	Light Industrial
P = Requires Plot Plan approval. C = Requires Conditional Use Permit approval. -- = Not Permitted				
All uses with P that have more than 200 square feet of outside storage or display of materials	C	C	C	P
Acid and abrasives manufacturing	--	C	C	C
Agricultural Uses of the Soils for Crops	--	--	P	--
Alcohol Sales	C	C	C	C
Ambulance services	P	P	P	P
Animal hospitals	C	C	C	C
Antique shops	P	P	P	--
Appliance manufacture and repair	P	P	P	P
Appliance stores, household	P	P	P	--
Art Gallery, Library, Reading Room, Museum*	--	P	--	--
Art supply shops and studios	P	P	P	--
Auditoriums and conference rooms (max 1,500 cap.)*	P	P	P	--
Automobile parts and supply stores	P	P	P	--
Automobile service and repair garages with or without body and fender shops or spray painting shops	C	C	C	P
Automobile sales and rental agencies	C	C	C	C
Automobile/gasoline service stations, not including the concurrent sale of beer and wine for off-premises consumption	P	P	P	P
Automobile/gasoline service stations, with the concurrent sale of beer and wine for off-premises consumption	C	C	C	C
Bakery goods distributors	P	P	P	P
Bakery shops, including baking only when incidental to retail sales on the premises	P	P	P	P
Banks and financial institutions	P	P	P	--
Barber and beauty shops	P	P	P	--
Bars and cocktail lounges	P	P	P	--
Bicycle sales and rentals	P	P	P	--
Billiard and pool halls	P	P	P	--
Binding of books and other publications	P	P	P	P
Blueprint and duplicating services	P	P	P	P
Boat sales, rentals and services	C	C	C	P

Land Uses	Commercial-Retail	Mixed Use	Business Park	Light Industrial
Book stores	P	P	P	P
Bowling alleys	P	P	P	--
Brewery, distillery, or winery	--	C	C	C
Building material sales yard	--	--	P	P
Building mover's storage yard	--	--	P	P
Car washes	C	C	C	C
Caretakers Unit as defined by Chapter 6 of the EMC.	--	C	P	--
Catering services	P	P	P	P
Cemeteries, crematories, and mausoleums	--	--	C	--
Ceramic sales and manufacturing for on-site sales, provided the total volume of kiln space does not exceed 16 cubic feet	P	P	P	--
Chemicals and related products manufacturing, not including pesticides and fertilizers	--	--	P	P
Churches, temples and other places of religious worship*	P	P	P	--
Cigar, Cigarette and Tobacco Sales		C	--	--
Cigar, Hookah, and Cigarette Lounges		C	--	--
Cleaning and dyeing shops	P	P	P	--
Clinics, including but not limited to medical, dental and chiropractic	P	P	P	--
Clothing stores	P	P	P	--
Coils, tubes, semiconductors and similar components	--	P	P	P
Cold storage facilities/plant	--	P	P	P
Communication, navigation control, transmission and reception equipment, control transmission and reception equipment, control equipment and systems, guidance equipment and systems	--	P	P	P
Communications and microwave installations*	--	P	P	P
Concrete Batch Plants and Asphalt Plants	--	--	C	--
Confectionery or candy stores	P	P	P	--
Contractor Storage Yards	--	P	P	--
Control devices and gauges	--	P	P	P
Convenience stores, including the sale of motor vehicle fuel	C	C	C	--
Convenience stores, not including the sale of motor vehicle fuel	P	P	P	--
Cotton, wood & synthetic weaving & finishing	--	P	P	P

Land Uses	Commercial-Retail	Mixed Use	Business Park	Light Industrial
mills				
Costume design studios	P	P	P	--
Cutlery, tableware, hand tools, and hardware manufacture	--	P	P	P
Dairy products, not including dairies	--	P	P	P
Data processing equipment and systems manufacture & repair	--	P	P	P
Day Care Centers*	P	--	P	--
Delicatessens	P	P	P	--
Department stores	P	P	P	--
Disposal Service Operations, not including Transfer Stations	--	--	--	C
Drive-in or Drive Through Operations or Facilities (any use)	C	C	P	C
Drive-in theaters*	C	C	C	--
Drug stores	P	P	P	--
Dry goods stores	P	P	P	--
Electrical products & components manufacturing	--	P	P	P
Emergency Shelters*	--	--	--	P
Employment agencies	P	P	P	
Engineering, survey, and drafting instruments manufacturing	--	P	P	P
Equipment rental services, including rototillers, power mowers, sanders, power saws, cement and plaster mixers not exceeding 20 cubic feet in capacity and other similar equipment	C	C	C	P
Equipment Sales and Storage	--	--	--	P
Farmers Markets	C	C	--	--
Feed and grain sales	P	P	P	P
Fertilizer production, organic or inorganic	--	--	C	C
Fire and police stations	--	P	P	P
Florist shops	P	P	P	--
Food markets and frozen food lockers	P	P	P	--
Food product manufacturing	--	P	P	P
Fortune-Telling, Spiritualism, or similar activity	--	P	P	--
Gift shops	P	P	P	--
Glassblowing, pressing, cutting, and other glassware products	--	P	P	P
Golf cart sales and service	P	P	P	P

Land Uses	Commercial-Retail	Mixed Use	Business Park	Light Industrial
Grocery Stores/Market/Food Store	P	--	--	--
Hardware stores and Home Improvement Centers	P	P	P	--
Hauling, Freightling, and Trucking Operations	--	--	--	C
Health and exercise centers provided all facilities are located within an enclosed building*	P	P	P	--
Hobby shops	P	P	P	--
Hotels, resort hotels and motels	P	P	P	--
Household goods sales and repair, including but not limited to, new and used appliances, furniture, carpets, draperies, lamps, radios, and television sets	P	P	P	P
Ice cream shops	P	P	P	--
Ice sales, not including ice plants	P	P	P	--
Ice manufacturing	--	P	P	P
Indoor Amusement/Entertainment Facility: Establishments providing indoor amusement and entertainment services as primary uses for a fee or admission charge, including dance halls and ballrooms and electronic game arcades. Establishments with four or more electronic games or coin-operated amusements, or where 50 percent or more of the floor area is occupied by amusement devices, are considered an electronic game arcade as described above; three or fewer machines are not considered a land use separate from the primary use of the site.*	P	P	P	--
Indoor Fitness and Sports Facility: Predominantly participant sports and health activities conducted entirely within an enclosed building. Typical uses include bowling alleys, billiard parlors, ice/roller skating rinks, indoor racquetball courts, indoor climbing facilities, soccer areas, athletic clubs, and health clubs*	P	P	P	P
Interior decorating shops	P	P	P	--
Jewelry manufacture and repair	--	P	P	P
Jewelry stores with incidental repairs	P	P	P	--
Kennel and Cattery, Class I, II, and III per the EMC	P	P	P	--
Kennel and Cattery, Class IV per the EMC	--	C	C	--

Land Uses	Commercial-Retail	Mixed Use	Business Park	Light Industrial
Labor temples	P	P	P	—
Laboratories, film, dental, medical, research or testing	P	P	P	P
Laundries and laundromats	P	P	P	--
Leather goods stores	P	P	P	—
Leather tanning and finishing	—	--	P	P
Liquid petroleum service stations, with or without the concurrent sale of beer and wine, provided that if storage tanks are provided above ground the total capacity of all tanks shall not exceed 10,000 gallons	C	C	C	C
Liquor stores	C	C	C	C
Locksmith shops	P	P	P	--
Lumber and wood products manufacturing	--	--	P	P
Lumber yards, including only incidental mill work	--	--	P	P
Machinery manufacturing	--	--	P	P
Mail order businesses	P	P	P	P
Manufacture and repair of engineering, scientific, and medical instrumentation	--	P	P	P
Manufacture of furniture and fixtures including cabinets, partitions, and similar items	--	P	P	P
Manufacture of handbags, luggage, footwear, and other personal leather goods	--	P	P	P
Manufacture of lighting fixtures, and supplies	--	P	P	P
Manufacture, assembly, testing and repair of components, devices, equipment and systems of an electrical, electronic, or electro-mechanical nature	--	P	P	P
Manufacturer's agent	P	P	P	--
Manufacturing, Limited: Limited manufacturing, fabricating, processing, packaging, treating, and incidental storage related thereto, provided any such activity shall be in the same line of merchandise or service as the trade or service business conducted on the premises.	—	P	P	P
Manufacturing, Major: Manufacturing, fabrication, processing, and assembly of materials in raw form. Uses in this category typically create greater than usual amounts of smoke, gas, odor, dust, sound, or other objectionable influences that might be	--	—	—	P

Land Uses	Commercial-Retail	Mixed Use	Business Park	Light Industrial
obnoxious to persons conducting business on-site or on an adjacent site. Uses include but are not limited to batch plants, rendering plants, aggregate processing facilities, plastics and rubber products manufacturing.				
Manufacturing, Minor: Manufacturing, fabrication, processing, and assembly of materials from parts that are already in processed form and that, in their maintenance, assembly, manufacture, or plant operation, do not create excessive amounts of smoke, gas, odor, dust, sound or other objectionable influences that might be obnoxious to persons conducting business on-site or on an adjacent site. Uses include but are not limited to furniture manufacturing and cabinet shops, laundry and dry cleaning plants, metal products fabrications, and food and beverage manufacturing.	--	P	P	P
Market, food, wholesale	P	P	P	P
Massage Parlors, Turkish Baths, or similar personal service establishments	C	P	--	--
Measuring devices, watches, clocks, and related items		P	P	P
Meat and Poultry Products, not including slaughtering	--	P	P	--
Meat markets, not including slaughtering	P	--	P	--
Meat packing plant, not including slaughtering or rendering of animals	--	P	C	C
Medical and dental instruments manufacture and repair	--	--	P	P
Metal building manufacturing	--	P	P	P
Metal products manufacturing including: assembled forged, stamped	--	P	P	P
Metering instruments, equipment and systems manufacture and repair	--	P	P	P
Mimeographing and addressograph services	P	P	P	--
Mini Storage	--	--	P	--
Mobile Home Sales Lot	--	P	P	--
Mobilehome and modular housing manufacturing	--	P	P	P

Land Uses	Commercial-Retail	Mixed Use	Business Park	Light Industrial
Mobilehomes, Construction offices and caretaker's quarters on construction sites for the duration of a valid building permit, providing they are inconspicuously located	P	P	P	--
Mobilehomes, provided they are kept mobile and licensed pursuant to state law, when used for construction offices and caretaker's quarters on construction sites for the duration of a valid building permit	P	C	P	P
Mortuaries	C	P	C	--
Music stores	P	P	P	--
Musical and recording equipment manufacture and repair	--	P	P	P
News stores	P	P	P	--
Nonalcoholic beverages manufacturing	--	P	P	P
Notions or novelty stores	P	P	P	--
Nurseries and garden supply stores	P	P	P	--
Office and computing machine manufacture, repair, and sales	--	P	P	P
Office equipment sales and service	--	P	P	P
Offices, professional sales and service, including business, law, medical, dental, chiropractic, architectural and engineering*	P	P	P	--
One-family dwellings on the same parcel as the industrial or commercial use provided such dwellings are occupied exclusively by the proprietor or caretaker of the use and their immediate families	P	P	P	P
Optical goods manufacture and repair	--	P	P	P
Paint and wall paper stores, not including paint contractors	P	P	P	--
Paints and varnishes manufacturing and incidental storage	--	--	C	C
Paper products manufacturing	--	--	P	P
Paper Storage and Recycling, not within a building	--	--	C	C
Parcel delivery services	--	P	--	P
Parking lots and parking structures	P	P	P	P
Pawn shops	P	P	P	--
Pet shops and pet supply shops	P	P	P	--
Pharmaceutical research and manufacture.	--	P	P	P

Land Uses	Commercial-Retail	Mixed Use	Business Park	Light Industrial
Phonographs, CD, DVD, and audio system manufacture & repair	--	P	P	P
Photo copying, duplicating, mimeographing and addressograph services	P	P	P	--
Photographic equipment manufacture & repair	--	P	P	P
Photography shops and studios and photo engraving	P	P	P	P
Plumbing shops, not including plumbing contractors	P	P	P	P
Poultry and egg processing	--	--	C	--
Post offices	--	P	P	P
Poultry markets, not including slaughtering or live sales	P	P	P	--
Prescription pharmacy alone or when related and incidental to a professional office building	P	P	P	--
Printers or publishers	P	P	P	P
Produce markets	P	P	P	
Professional Offices*	P	P	P	P
Public utility substations and storage buildings	--	P	--	P
Radar, infrared and ultraviolet equipment and systems manufacture & repair	--	P	P	P
Radio and television broadcasting studios	P	P	P	--
Recording studios	P	P	P	--
Recycling collection facilities*	P	P	P	P
Recycling processing facilities	--	C	C	P
Recycling of wood, and metal	--	--	C	P
Recycling of construction wastes and other materials	--	C	C	C
Refreshment stands	P	P	P	--
Restaurants and other eating establishments, including drive-thrus*	P	P	P	--
Retail Sales and Services, Small Scale per the EMC	P	P	P	C
Sale, rental, repair, or demonstration of motorcycles, scooters or motorbikes of two horsepower or greater	C	C	C	--
Schools/studios - business and professional, including: fine arts, barber, beauty, dance, drama, music, photography, and swimming, where no stock of goods is maintained for sale*	P	P	P	--

Land Uses	Commercial-Retail	Mixed Use	Business Park	Light Industrial
Scientific and mechanical instruments manufacture & repair	—	P	P	P
Self-storage facilities, including mini-warehouses	C	C	P	P
Sex-oriented businesses, subject to the provisions of R the EMC	—	**	**	**
Shoe stores and repair shops	P	P	P	—
Shoeshine stands	P	P	P	—
Signs, on-site advertising	P	P	P	P
Sporting goods stores	P	P	P	
Sports and recreational facilities, not including motor-driven vehicles and riding academies, but including: archery ranges, athletic fields, beaches, golf driving ranges, gymnasiums, miniature golf, parks, playgrounds, sports arenas, skating rinks, stadiums, and commercial swimming pools with an occupancy capacity less than 1,500 persons*	C	C	C	—
Stained glass assembly	P	P	P	P
Stationery stores	P	P	P	—
Stations, bus, railroad and taxi	P	P	P	—
Stone, clay, glass and concrete products manufacturing	—	P	P	P
Sugar and confectionary products manufacturing	—	P	P	P
Tailor shops	P	P	P	—
Tattoo Parlors	C	C	—	—
Taxidermist	P	P	P	—
Telephone exchanges	P	P	P	P
Television and radio equipment and systems manufacture & repair	—	P	P	P
Theaters, not including drive-ins*	P	P	P	—
Tire recapping	C	C	C	C
Tire sales and services, not including recapping	C	C	C	C
Tobacco shops	P	P	P	—
Tourist information centers	P	P	P	—
Toy shops	P	P	P	—
Travel agencies	P	P	P	—
Travel trailer and boat storage	C	P	P	P
Travel trailers, mobilehomes and recreational vehicles manufacture	—	P	P	P

Land Uses	Commercial-Retail	Mixed Use	Business Park	Light Industrial
Travel trailers, mobilehomes and recreational vehicles sales and service	C	C	C	C
Truck sales and services	C	C	C	C
Truck wash	C	C	P	--
Trucks and trailers rental	C	C	P	C
Typewriter sales and rental and incidental repairs	P	P	P	--
Underground bulk fuel storage	--	C	C	C
Vehicle Storage and Impoundment within an enclosed building	--	P	P	P
Vehicle Storage and Impoundment, Outdoor	--	--	C	--
Vehicles manufacturing	--	P	P	P
Vehicles repair shops	--	P	P	P
Vehicles, Aircraft, Boats and Parts Manufacture	--	P	P	P
Warehousing and distribution	--	--	P	P
Warehousing and distribution ancillary to uses permitted herein	--	P	P	P
Watch repair shops	P	P	P	--
Water and gas company service facilities	--	--	--	--
Wearing apparel and accessory products manufacturing	--	P	P	P
Wedding chapels*	P	P	P	--
Wholesale businesses with samples on the premises, but not to include storage	P	P	P	--

* Prior to application for any of these potentially high intensity uses, consideration of the size and location of such uses should be made in relation to the Chino Airport Land Use Compatibility Plan and the applicable intensity criteria.

** Allowed only pursuant to the Eastvale Municipal Code (EMC).

2.2 Project-Wide Development Plans and Standards

The following sections illustrate and describe the project-wide development plans and standards as they relate to land use, circulation, drainage systems, landscaping, water and sewer systems, phasing, grading, and comprehensive maintenance for the entire Specific Plan area. Project-wide development standards have been prepared to complement the standards applicable to each individual Planning Area as discussed in detail in Section 2.6. Project-wide standards are set forth below and in **Table 2-3, Development Standards.**

Development Standards

- 1) The Ranch at Eastvale Specific Plan shall be developed in general conformance with the Land Use Plan (Figure 2-1). General permitted uses will include Commercial-Retail, Business Park, Light Industrial, and other uses as delineated in Table 2-2, for the individual Planning Areas.
- 2) Planning areas may be developed with total square footages that are above or below the planning areas' identified development target and with acreages that are above or below the planning areas' identified size through compliance with the procedures set forth in Section 2.5, Specific Plan Administration provided the maximum building square footage within The Ranch at Eastvale Specific Plan does not exceed 1,812,274 square feet without an amendment of the Specific Plan.
- 3) Uses and development standards will be in accordance with the City of Eastvale Municipal Zoning Code as amended by The Ranch at Eastvale Specific Plan Zoning Ordinance and will be further defined by Specific Plan objectives, the Specific Plan design guidelines, and future detailed development proposals including subdivisions, plot plans, and conditional use permits.
- 4) Standards and guidelines relating to signs, landscaping, parking, and other related design elements will conform to the City of Eastvale Municipal Zoning Code unless addressed by the guidelines and standards within The Ranch at Eastvale Specific Plan.
- 5) All project lighting shall be in conformance with applicable City of Eastvale standards. Prior to the issuance of a building permit, a photometric light plan will be submitted for review and approval to ensure that proper outdoor light, pursuant to City of Eastvale Municipal Code Section 120.05.050, Outdoor Lighting, is provided. This site specific development application shall meet the following standards:
 - a) Parking lots, driveways, trash enclosures/areas, and mailboxes shall be illuminated with a minimum of 1 foot-candle and an average not to exceed 4 foot-candles.
 - b) Pedestrian walkways shall be illuminated with a minimum of ½ foot-candle and an average not to exceed 2 foot-candles.
 - c) Entry and exterior doors shall be illuminated with a minimum of 1 foot-candle measured within a 5 foot radius of each side of the door at ground level.

- 6) Development of the property shall be in accordance with the mandatory requirements of all the City of Eastvale ordinances and state laws; and shall conform substantially to The Ranch at Eastvale Specific Plan as filed in the office of the City of Eastvale Planning Department, unless otherwise amended.
- 7) The evaluation of environmental impacts for The Ranch at Eastvale Specific Plan is contained in Environmental Impact Report No. 498 prepared for Specific Plan No. 358. Pursuant to Section 15183 of the State CEQA Guidelines, implementing development projects which are consistent with the development density established by The Ranch at Eastvale Specific Plan shall not require additional environmental review, except as might be necessary to examine whether there are project-specific significant effects which are peculiar to the project.
- 8) Except for the Specific Plan Development Standards, Design Guidelines, Landscape Guidelines, and other requirements identified in Sections 2.0 through 4.0 that have been adopted concurrently with this Specific Plan, no portions of the Specific Plan which purport or propose to change, waive, or modify any ordinance or other legal requirement for development shall be considered to be part of the adopted Specific Plan.
- 9) Lots created pursuant to this Specific Plan and subsequent tentative maps shall be in conformance with the development standards of the zoning applied to the Specific Plan, and all other applicable City standards and the Subdivision Map Act.
- 10) For the security and safety of future users of the facilities constructed within the Specific Plan, the developer shall consider the following design concepts within each individual development proposal:
 - a) Circulation for pedestrians, vehicles, and police patrols (commercial uses only).
 - b) Lighting of streets, walkways, and bikeways.
 - c) Visibility of doors and windows from the street and between buildings, where practical.
 - d) Fencing of appropriate heights and materials.
- 11) The following crime prevention measures shall also be considered during the individual site and building layout design, in addition to those above, for the security and safety of future occupants of light industrial, office, and commercial space:
 - a) Addresses which light automatically at night.
 - b) Installation of burglar alarms in all commercial buildings.
 - c) Special lighting requirements on any buildings that are grouped in a way that individual addresses are difficult to read.
- 12) A land division filed for the purposes of phasing or financing shall not be considered an implementing development application, provided that, if the maintenance organization is a property owners' association, the legal documentation necessary to establish the association shall be recorded prior to the issuance of occupancy permits.

- 13) Each planning area shall comply with applicable City of Eastvale recycling requirements.
- 14) On-site commercial areas shall contain enclosures for collection of recyclable materials.
- 15) Construction and occupancy of the Specific Plan including roads and infrastructure may be done progressively in stages, provided vehicular access, public facilities and infrastructure are constructed to adequately service each phase of development or as needed for public health and safety in each stage of development and further provided that each phase of development conforms substantially with the intent and purpose of Section 2.2.6, Public Facilities and Phasing Requirements.
- 16) Construction of required infrastructure such as sewer and water lines, storm drains and roads may be financed through the establishment of a financing district (e.g., assessment district, Community Facilities District).
- 17) Prior to recordation of a final map, issuance of building permits, or conveyance to an entity exempt from the Subdivision Map Act, whichever occurs first, the landowner shall convey an avigation easement to Chino Airport.
- 18) In order to meet the 2008 Chino Airport Land Use Compatibility Plan requirement that development within Compatibility Zone B1 provide 30 percent open land, development within Compatibility Zone C provide 20 percent open land, and development within Compatibility Zone D provide 10 percent open land; a minimum of 24 acres of open land as defined by Countywide Policy 4.2.4 of the 2004 Riverside County Airport Land Use Compatibility Plan shall be provided within the Specific Plan. Such open land includes public and private streets and parking lots. The open land shall have minimum dimensions of approximately 75 feet by 300 feet, and shall be free of most structures and other major obstacles such as walls, large trees or poles greater than 4-inches in diameter measured at 4 feet above the ground, or overhead wires.
 - a) In the case of phased development, excess approved open space under ALUC guidelines may be transferred to a future phase of development within the same ALUC compatibility zone.
- 19) The following uses shall be prohibited within the Specific Plan:
 - a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - b) Any use which would cause sunlight to be reflected toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport.

- c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which otherwise may affect safe air navigation within the area.
 - d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
- 20) Public and private schools for grades K through 12; assembly facilities exceeding 1,500 capacity, hospitals and hospices, child care facilities, nursing homes, disposal service operations and compostable material handling facilities shall not be permitted. Churches, temples and other places used primarily for religious worship shall be permissible only in Planning Area 5, and portions of Planning Areas 6 and 9.
- 21) The City may initiate an amendment or revocation proceeding on all or any portion of this Specific Plan if a development proposal for all or any portion of the Specific Plan has not been submitted within twenty (20) years of the City Council's adoption of the Specific Plan.

Table 2-3, Development Standards

Standards	Commercial-Retail	Mixed-Use	Business Park	Light Industrial				
Building Specifications								
Minimum Lot Size	No Minimum	No Minimum	10,000 s.f.	20,000 s.f.				
Minimum Average Lot Width	No Minimum	No Minimum	75 feet	100 feet				
Maximum Building Size	No Maximum	No Maximum	125,000 s.f.	300,000 s.f.				
Setbacks								
A) Buildings/Parking Areas								
1. A minimum building setback shall be required for each street frontage. The building setbacks listed in Item B) Streets, below are the average building setback. The minimum building setback is the same as below-listed minimum parking setback. All setbacks are measured from the edge of the right-of-way. The setback strip adjacent to the street shall be appropriately landscaped and maintained, except for designated pedestrian and vehicular access ways.								
B) Streets	Avg. Bldg.	Min. Parking	Avg. Bldg.	Min. Parking	Avg. Bldg.	Min. Parking	Avg. Bldg.	Min. Parking
Limonite Avenue frontage	19 ft.	9 ft.	19 ft.	9 ft.	19 ft.	9 ft.	19 ft.	9 ft.
Hellman Avenue frontage	12 ft.	7 ft.	12 ft.	7 ft.	12 ft.	7 ft.	12 ft.	7 ft.
Interior Street frontage	14 ft.	9 ft.	14 ft.	9 ft.	14 ft.	9 ft.	14 ft.	9 ft.
C) Planning Areas								
1. Where the front, side or rear yard within an Light Industrial planning area adjoins a commercially-zoned lot and where the front, side or rear yard within any planning area adjoins a lot zoned R-R, R-1, R-A, R-2, R-3, R-4, R-6, R-T, R-T-R, or W-2-M, the minimum building setback from the property line shall be:								
Front	25 feet	25 feet	25 feet	50 feet				
Side	25 feet	25 feet	25 feet	50 feet				
Rear	25 feet	25 feet	25 feet	50 feet				
2. A minimum 50 foot building setback shall be required on any boundary where the industrial property abuts a residential or commercially zoned property. A minimum of 20 feet of the setback shall be landscaped, unless a tree screen is approved, in which case the setback area may be used for automobile parking, driveways or landscaping. Block walls or other fencing may be required. [See also Landscape Yard Setbacks, Item C.2, below]								
3. Where the front, side or rear yard adjoins a lot zoned a classification other than R-R, R-1, R-A, R-2, R-3, R-4, R-6, R-T, R-T-R, or W-2-M, the minimum building setback measured from the front, side and rear property line shall be:								
Front	No Minimum	No Minimum	No Minimum	No Minimum				
Side	No Minimum	No Minimum	No Minimum	10 feet for the two side lot areas combined				
Rear	No Minimum	No Minimum	No Minimum	15 feet				
C) Landscaped Yard Setbacks								
1. A minimum landscape strip adjacent to street right-of-way lines, as permitted under Building/Parking Areas Item A.1 above, shall be appropriately landscaped and maintained, except for designated pedestrian and vehicular access ways. Said landscaped strip shall not include landscaping located within the street right-of-way								
2. A minimum 20 foot strip adjacent to lots zoned R-R, R-1, R-A, R-2, R-3, R-4, R-6, R-T, or PRD, or separated by a street from a lot with said zoning, shall be landscaped and maintained, unless a								

Standards	Commercial-Retail	Mixed-Use	Business Park	Light Industrial
tree screen or other buffer treatment is approved by the hearing officer or body. However, in no case shall said landscaping be less than ten feet wide excluding curbing.				
Height Requirements^{1, 2, 3}				
Maximum building height	50 feet	50 feet	50 feet	50 feet
Maximum structure height at the yard setback line.	35 Feet	40 feet	45 feet	45 feet
<ol style="list-style-type: none"> Any portion of a building which exceeds the setback line height shall be set back from the front, rear and side lot lines not less than two feet for each foot by which the height exceeds 35 feet. A greater height is approvable pursuant to Section 5.1 of the City of Eastvale Municipal Zoning Code. In no event, however, shall a building or structure exceed seventy-five (75') feet in height, or 105. Buildings within ALUC Compatibility Zone B1 shall have no more than 2 habitable floors and buildings within Zone C shall have no more than 3 habitable floors. 				
Masonry Walls¹				
Minimum Height	6 to 8 feet	6 to 8 feet	6 to 8 feet	6 to 8 feet
<ol style="list-style-type: none"> A solid masonry wall or combination landscaped earthen berm and masonry wall of the minimum height specified herein, shall be constructed on each property line that adjoins any parcel specifically zoned for residential use. 				
Landscaping				
Minimum Coverage	15%	15%	10%	10%
Parking				
Parking areas shall be provided as required by Section 5.6 of the Eastvale Municipal Code and as identified in Section 2.2.3, Conceptual Landscape Development Standards, below.				
Trash Collection Areas				
Trash receptacles (including recycling and green waste containers) shall not be stored within a required front or street side yard and shall be screened from view of the public right-of-way by a solid fence not less than four feet in height. Exceptions to fence height-standards may be granted by the designated approving authority to ensure proper placement and screening of trash receptacles.				
Screening				
Parking, loading, and service areas: Shall be screened by structures or landscaping. They shall be located in such a manner as to minimize noise or odor nuisance. Block walls or other fencing may be required.				
Utilities: All new utilities shall be installed underground except that electrical lines rated at 33kv or greater may be installed above ground.				
Mechanical Equipment: All roof mounted mechanical equipment shall be screened from the ground elevation view to a minimum sight distance of 1,320 feet. Mechanical equipment used in the manufacturing process shall be required to be enclosed in a building.				
Outside storage areas: Shall be screened with structures or landscaping. Landscaping shall be placed in a manner adjacent to the exterior boundaries of the area so that materials stored are screened from view. If a non-screened exhibit of products is proposed, it shall be part of the industrial park plot plan, and shall be set back at least ten feet from the street line				
Lighting				
All lighting fixtures, including spot lights, electrical reflectors and other means of illumination for signs, structures, landscaping, parking, loading, unloading and similar areas, shall be focused, directed, and arranged to prevent glare or direct illumination on streets or adjoining property.				

2.5 Airport Zones

Chino Airport Land Use Compatibility

The Ranch at Eastvale Specific Plan is located approximately 3/4 mile east of the Chino Airport, a general aviation transport airport. The Chino Airport Comprehensive Land Use Plan (CLUP) is a policy document intended to protect the health and safety of residents in the airport vicinity and users of the airport. The Ranch at Eastvale Specific Plan is located in an area subject to review by the Riverside County Airport Land Use Commission (ALUC). The CLUP designates an airport influence area and includes land use compatibility guidelines that address airport noise, safety, height restrictions, and general concerns related to aircraft overflight. The "airport influence area" around the Chino Airport is divided into six land use compatibility zones. Three compatibility zones affect the Specific Plan. A majority of the Specific Plan area is located within Zone C, with the exception of the northern portion of the site which is located within Zone B-1 and the southern portion of the site which is located within Zone D as reflected on **Figure 2-10, ALUC Compatibility Zones.**

The CLUP establishes land use compatibility guidelines for land use compatibility zones that affect the Specific Plan area. These guidelines are summarized in **Table 2-6, Land Use Compatibility Guidelines for Chino Airport Safety Zones** and **Table 2-7, Chino Airport Land Use Compatibility Zones.** All development shall be consistent with the Chino Airport CLUP.

The Ranch at Eastvale Specific Plan is consistent with the land use compatibility guidelines for noise, safety and height contained in the CLUP. However approval from ALUC will be required prior to any public hearing, or staff level approval for any implementing development application, grading permit, or pad elevation. ALUC shall be responsible for reviewing any development application for consistency with the Chino Airport Land Use Compatibility Plan and to determine whether FAA review for Obstruction Evaluation may be required.

Table 2-6, Land Use Compatibility Guidelines for Chino Airport Safety Zones

Safety Zone	Maximum Population Density	Maximum Coverage by Structures	Land Use ^{1,2,3}
Area I	0 ³	0 ³	No significant obstructions ⁴ No petroleum or explosives No above-grade powerlines
Area II	<i>Uses in structures:</i> ⁵ 25 persons per acre OR 150 persons per building <i>Uses not in structures:</i> 50 persons per acre Residential: 2.5 acre minimum lots <i>Uses in Structure:</i> ⁵ 75 persons per acre OR 300 persons per building	25% of net area 50% of gross area or 65% of net area whichever is greater	No residential No hotels, motels No restaurants, bars No schools, hospitals, government services No concert halls, auditoriums No stadiums, arenas No public utility stations, plants No public communications facilities No uses involving, as the primary activity, manufacture, storage, or distribution of explosives or flammable materials. ⁶
Area III	Not Applicable	50% of gross area or 65% of net area whichever is greater	Discourage schools, auditorium, amphitheatres, stadiums Discourage uses involving, as the primary activity, manufacture, storage, or distribution of explosives or flammable materials. ⁶

Source: County of Riverside General Plan Eastvale Area Plan, Table 4
 NOTES:

1. The following uses shall be prohibited in all airport safety zones:
 - a. Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - b. Any use which would cause sunlight to be reflected toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport.
 - c. Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which otherwise may affect safe air navigation within the area.
 - d. Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
2. Avigation easements shall be secured through dedication for all land uses permitted in safety zones.
3. No structures permitted in ETZ or ISZ.
4. Significant obstructions include but are not limited to large trees, heavy fences and walls, tall and steep berms and retaining walls, non-fragile street light and sign standards, billboards.
5. A "structure" includes fully enclosed buildings and other facilities involving fixed seating and enclosures limiting the mobility of people, such as sports stadiums, outdoor arenas, and amphitheatres.
6. This does not apply to service stations involving retail sales of motor vehicle fuel if fuel storage tanks are installed underground.

Table 2-7, Chino Airport Land Use Compatibility Zones^A

Zone	Locations	Maximum Densities / Intensities				Additional Criteria		
		Residential (d.u./ac) ¹	Other Uses (people/ac) ²			Req'd Open Land ³	Prohibited Uses ⁴	Other Development Conditions ⁵
		Aver- age ⁶	Single Acre ⁷	with Bonus ⁸				
A	Runway Protection Zone and within Building Restriction Line	0	0	0	0	All Remaining	All structures except ones with location set by aeronautical function Assemblages of people Objects exceeding FAR Part 77 height limits Storage of hazardous materials Hazards to flight ⁹	Avigation easement dedication
B1	Inner Runway Protection Zone	0.05 (average parcel size ≥20.0 ac.) ⁴	40	80	104	30%	Children's schools, day care centers, libraries Hospitals, nursing homes Places of worship Bldgs with >2 aboveground habitable floors Highly noise-sensitive outdoor nonresidential uses ¹⁰ Aboveground bulk storage of hazardous materials ¹¹ Critical community infrastructure facilities ¹² Hazards to flight ⁹	Locate structures maximum distance from extended runway centerline Minimum NLR of 25 dB in residences (including mobile homes) and office buildings ¹³ Airspace review required for objects >35 feet tall ¹⁴ Avigation easement dedication
B2	Adjacent to Runway	0.1 (average parcel size ≥10.0 ac.)	100	200	260	No Req't	Same as Zone B1	Locate structures maximum distance from runway Minimum NLR of 25 dB in residences (including mobile homes) and office buildings ¹³ Airspace review required for objects >35 feet tall ¹⁴ Avigation easement dedication
C	Extended Approach/Departure Zone	0.2 (average parcel size ≥5.0 ac.)	75	150	195	20%	Children's schools, day care centers, libraries Hospitals, nursing homes Bldgs with >3 aboveground habitable floors Highly noise-sensitive outdoor nonresidential uses ¹⁰ Hazards to flight ⁹	Minimum NLR of 20 dB in residences (including mobile homes) and office buildings ¹³ Airspace review required for objects >70 feet tall ¹⁵ Deed notice required
D	Primary Traffic Patterns and Runway Buffer Area	(1) ≤0.2 (average parcel size ≥5.0 ac.) or ¹⁶ (2) ≥5.0 (average parcel size ≤0.2 ac.)	150	450	585	10%	Highly noise-sensitive outdoor nonresidential uses ¹⁰ Hazards to flight ⁹	Airspace review required for objects >70 feet tall ¹⁵ Children's schools, hospitals, nursing homes discouraged ¹⁷ Deed notice required
E	Other Airport Environs	No Limit	No Limit ¹⁸		No	No Req't	Hazards to flight ⁹	Airspace review required for objects >100 feet tall ¹⁵ Major spectator-oriented sports stadiums, amphitheaters, concert halls discouraged beneath principal flight tracks ¹⁹
*	Height Review Overlay	Same as Underlying Compatibility Zone			Not Applicable		Same as Underlying Compatibility Zone	Airspace review required for objects >35 feet tall ¹⁴ Avigation easement dedication

See Chapter 3 for airport-specific additions or exceptions to these policies

Source: Riverside County Airport Land Use Commission Compatibility Plan Policy Document, as adopted October 2004, Table 2A-Basic Compatibility Criteria, as modified per Chapter 9-Chino Airport, page 3-10b.

NOTES:

- A Policies numbered below are applicable to Table 2-7 unless otherwise noted by the most recently adopted Chino Airport CLUP.
- 1 Residential development must not contain more than the indicated number of dwelling units (excluding secondary units) per gross acre. Clustering of units is encouraged. See Policy 4.2.5 for limitations. Gross acreage includes the property at issue plus a share of adjacent roads and any adjacent, permanently dedicated, open lands. Mixed-use development in which residential uses are proposed to be located in conjunction with nonresidential uses in the same or adjoining buildings on the same site shall be treated as nonresidential development. See Policy 3.1.3(d).
- 2 Usage intensity calculations shall include all people (e.g., employees, customers/visitors, etc.) who may be on the property at a single point in time, whether indoors or outside.
- 3 Open land requirements are intended to be applied with respect to an entire zone. This is typically accomplished as part of a community general plan or a specific plan, but may also apply to large (10 acres or more) development projects. See Policy 4.2.4 for definition of open land.
- 4 The uses listed here are ones that are explicitly prohibited regardless of whether they meet the intensity criteria. In addition to these explicitly prohibited uses, other uses will normally not be permitted in the respective compatibility zones because they do not meet the usage intensity criteria.
- 5 As part of certain real estate transactions involving residential property within any compatibility zone (that is, anywhere within an airport influence area), information regarding airport proximity and the existence of aircraft overflights must be disclosed. This requirement is set by state law. See Policy 4.4.2 for details. Easement dedication and deed notice requirements indicated for specific compatibility zones apply only to new development and to reuse if discretionary approval is required.
- 6 The total number of people permitted on a project site at any time, except rare special events, must not exceed the indicated usage intensity times the gross acreage of the site. Rare special events are ones (such as an air show at the airport) for which a facility is not designed and normally not used and for which extra safety precautions can be taken as appropriate.
- 7 Clustering of nonresidential development is permitted. However, no single acre of a project site shall exceed the indicated number of people per acre. See Policy 4.2.5 for details.
- 8 An intensity bonus may be allowed if the building design includes features intended to reduce risks to occupants in the event of an aircraft collision with the building. See Policy 4.2.6 for details.
- 9 Hazards to flight include physical (e.g., tall objects), visual, and electronic forms of interference with the safety of aircraft operations. Land use development that may cause the attraction of birds to increase is also prohibited. See Policy 4.3.7.
- 10 Examples of highly noise-sensitive outdoor nonresidential uses that should be prohibited include amphitheatres and drive-in theaters. Caution should be exercised with respect to uses such as poultry farms and nature preserves.
- 11 Storage of aviation fuel and other aviation-related flammable materials on the airport is exempted from this criterion. Storage of up to 6,000 gallons of nonaviation flammable materials is also exempted. See Policy 4.2.3(c) for details.
- 12 Critical community facilities include power plants, electrical substations, and public communications facilities. See Policy 4.2.3(d) for details.
- 13 NLR = Noise Level Reduction, the outside-to-inside sound level attenuation that the structure provides. See Policy 4.1.6.
- 14 Objects up to 35 feet in height are permitted. However, the Federal Aviation Administration may require marking and lighting of certain objects. See Policy 4.3.6 for details.
- 15 This height criterion is for general guidance. Shorter objects normally will not be airspace obstructions unless situated at a ground elevation well above that of the airport. Taller objects may be acceptable if determined not to be obstructions. See Policies 4.3.3 and 4.3.4.
- 16 Two options are provided for residential densities in *Compatibility Zone D*. Option (1) has a density limit of 0.2 dwelling units per acre (i.e., an average parcel size of at least 5.0 gross acres). Option (2) requires that the density be greater than 5.0 dwelling units per acre (i.e., an average parcel size less than 0.2 gross acres). The choice between these two options is at the discretion of the local land use jurisdiction. See Table 2B for explanation of rationale. All other criteria for *Zone D* apply to both options.
- 17 Discouraged uses should generally not be permitted unless no feasible alternative is available.
- 18 Although no explicit upper limit on usage intensity is defined for *Zone E*, land uses of the types listed—uses that attract very high concentrations of people in confined areas—are discouraged in locations below or near the principal arrival and departure flight tracks. This limitation notwithstanding, no use shall be prohibited in *Zone E* if its usage intensity is such that it would be permitted in *Zone D*.

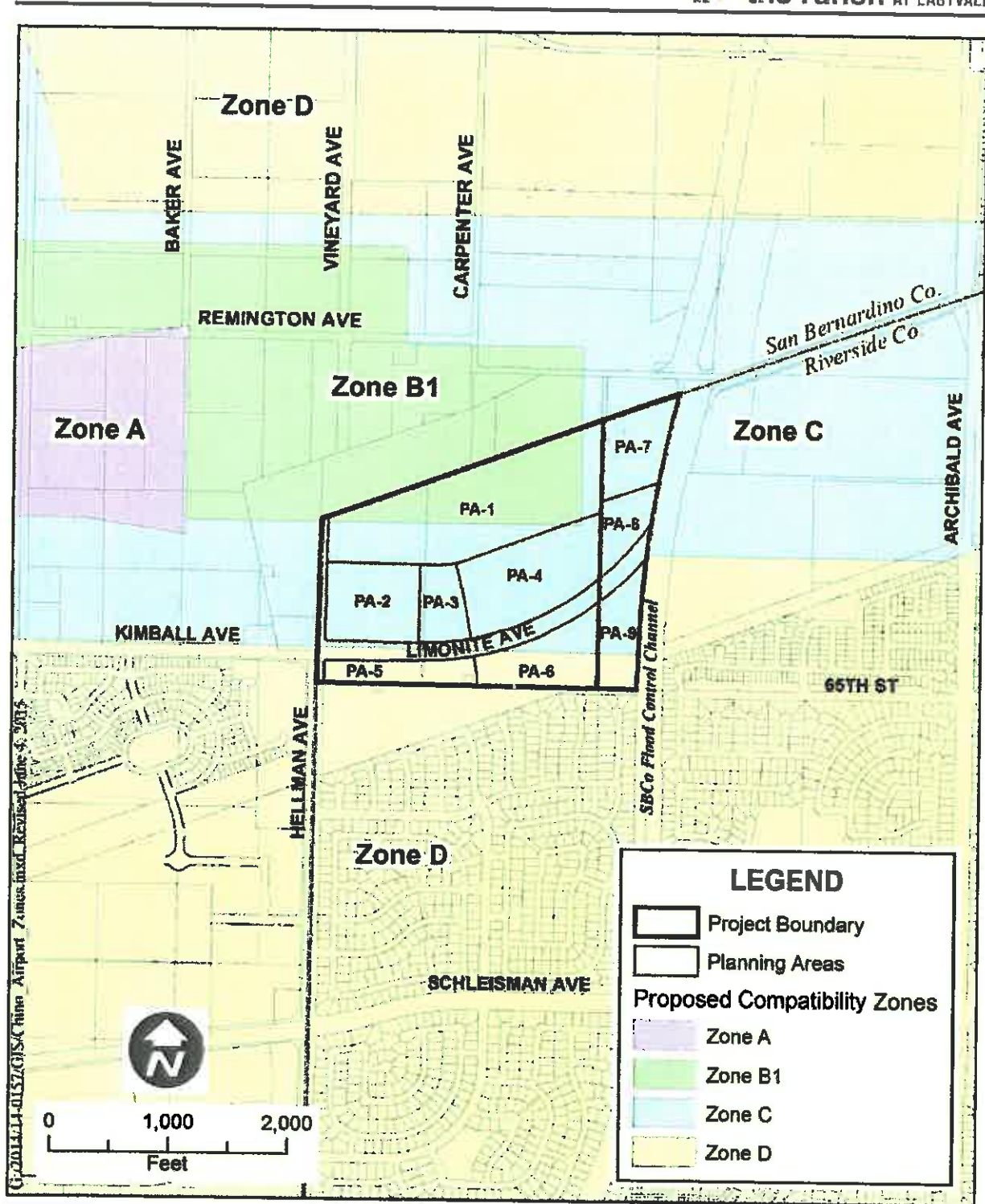


Figure 2-10
ALUC Compatibility Zones

2.7 Land Use Planning and Design Standards by Planning Area

In order to ensure the orderly and sensible development of the land uses proposed for The Ranch at Eastvale Specific Plan, land use planning and design standards have been created for each Planning Area. These planning area-specific standards, in addition to the project-wide standards, will assist in accommodating the proposed development and provide adequate transitions to neighboring land uses.

2.7.1 Planning Area 1: Light Industrial

Planning Area 1 located along the northern project boundary, depicted in **Figure 2-11, Planning Area 1**, provides for development of an area with approximately 36.1 acres for light industrial land uses. A maximum 644,000 square feet of buildings can be constructed within Planning Area 1 with building sizes not to exceed 300,000 square feet. The building intensity in Planning Area 1 will not exceed a FAR of 0.60.







Land Use and Development Standards

For permitted land uses and development standards such as setbacks and yard requirements, refer to Table 2-2, Allowable Land Uses and Table 2-3, Development Standards, except as otherwise provided for in the text and exhibits of this Specific Plan.

Planning Standards

- 1) Access to Planning Area 1 shall be provided from Hellman Avenue and/or any future Internal Street(s) providing access into the project area. Future development design may permit internal access between Planning Area 1 and adjoining Planning Areas.
- 2) Planning Area 1 is located in Zones B1 and C of the CLUP for the Chino Airport. Development in these zones will comply with all applicable restrictions and requirements.
- 3) Pedestrian access is by way of sidewalks along roads and internal pathways to individual businesses that provide connections throughout the project.
- 4) The Ranch at Eastvale Secondary Monument shall be provided at the southwest corner of the planning area and at the northeast corner of the planning area, as described in Section 3.6.
- 5) The Community of Eastvale Monument shall be provided at the northeast corner of the planning area, as described in Section 3.6.
- 6) A Tubular Steel Fence, as shown on *Figure 3-7, Fence and Wall Design*, shall be provided along the northern boundary of Planning Area 1.
- 7) Please refer to Section 3.0 for specific Design Guidelines and other related design criteria and to Section 4.0 for specific Landscape Guidelines.
- 8) Please refer to Sections 2.2 through 2.5 for Development Plans and Standards that apply site-wide.

LEGEND

-  TUBULAR STEEL FENCE
-  THE RANCH AT EASTVALE SECONDARY MONUMENT
-  THE COMMUNITY OF EASTVALE MONUMENT
-  5' MIN. SIDEWALK - PEDESTRIAN CIRCULATION
-  INDUSTRIAL COLLECTOR ENTRIES
-  PLANNING AREA ACCESS

Planning Area 1
LIGHT INDUSTRIAL
36.1 Acres

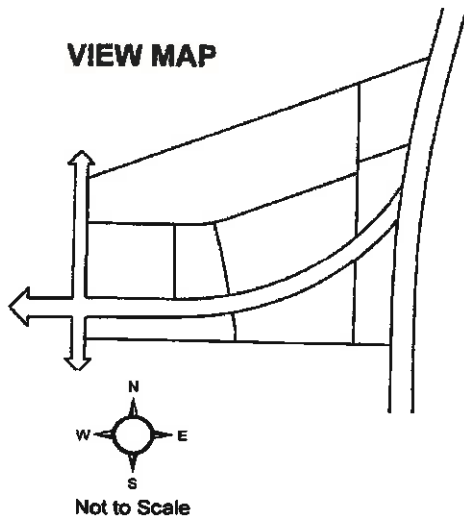
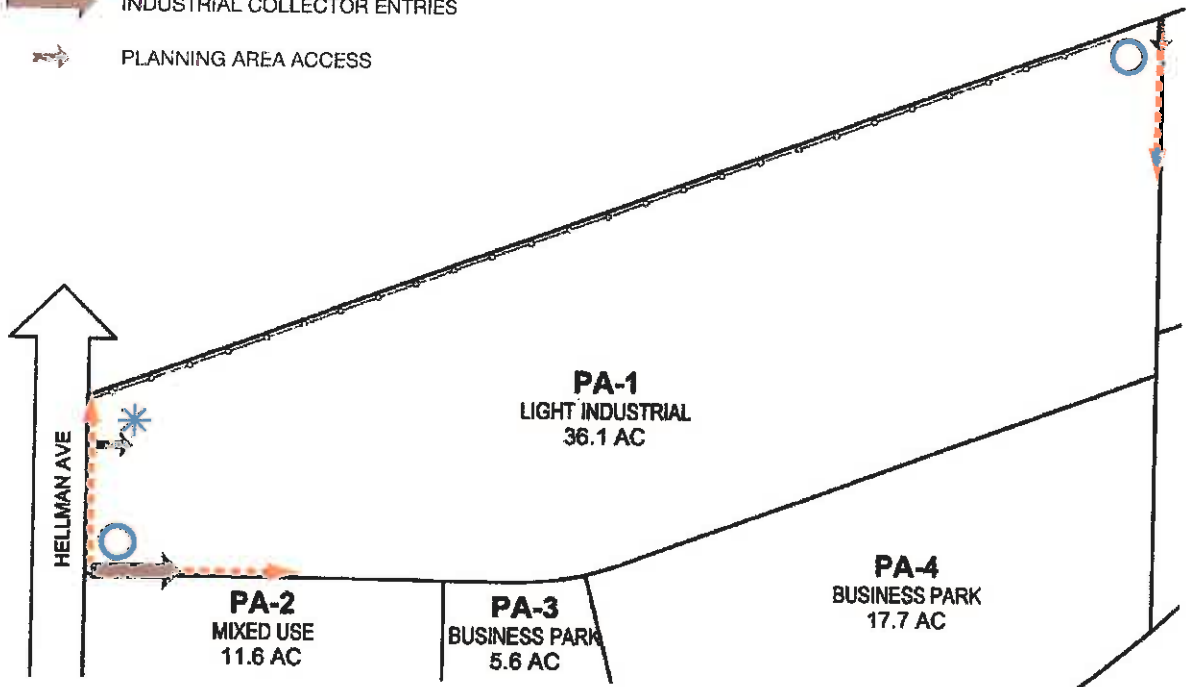


Figure 2-11
Planning Area 1

2.7.2 Planning Area 2: Mixed-Use

Planning Area 2, located along the western project boundary, along the east side of Hellman Avenue and north of Limonite Avenue as depicted in **Figure 2-12, Planning Area 2**, provides for the development of approximately 11.6 acres of mixed-use land uses. Planning Area 2 shall develop with a minimum of 50 percent commercial-retail type land uses with no limitation on building size. A maximum of 160,000 square feet of buildings can be constructed within Planning Area 2. The building intensity in Planning Area 2 will not exceed a FAR of 0.35.








Land Use and Development Standards

For permitted land uses and development standards such as setbacks and yard requirements, refer to Table 2-2, Allowable Land Uses and Table 2-3, Development Standards, except as otherwise provided for in the text and exhibits of this Specific Plan.

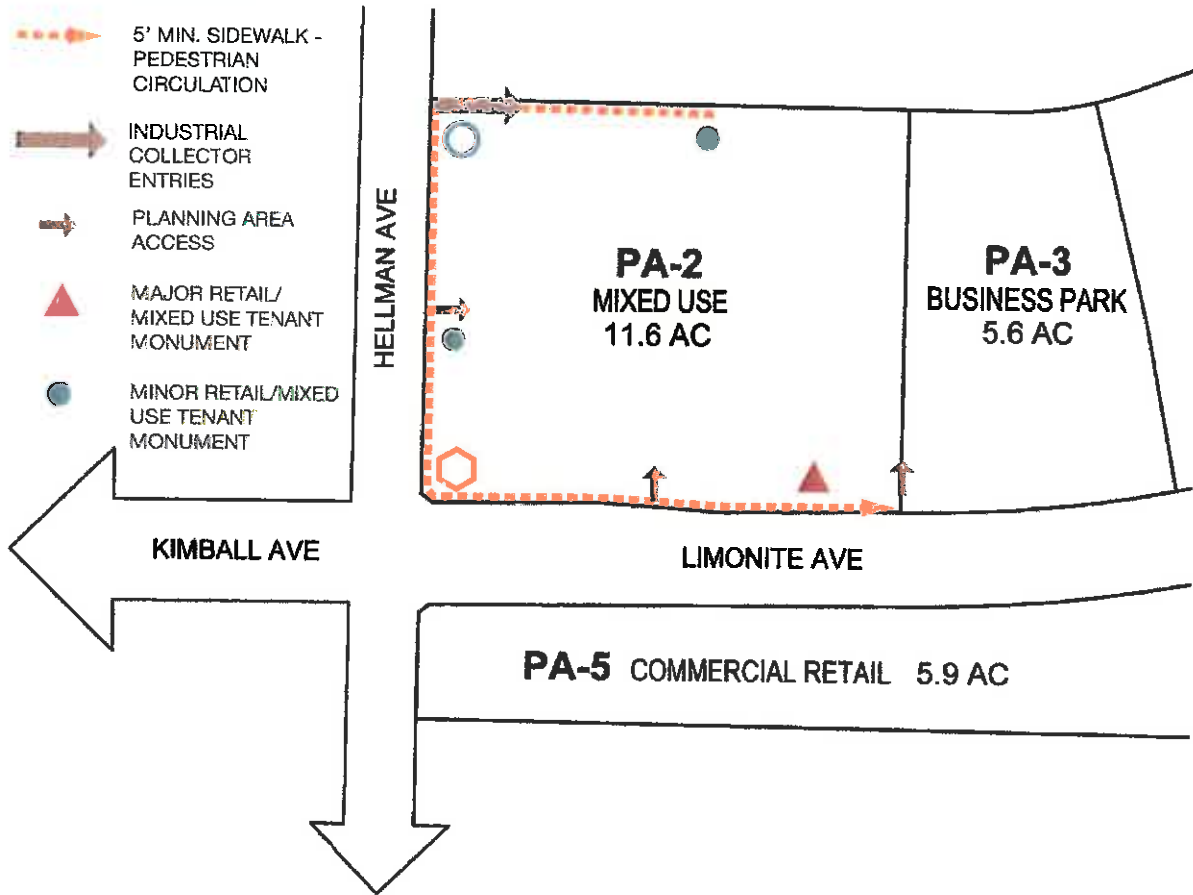
Planning Standards

- 1) Access to Planning Area 2 shall be provided from Hellman Avenue, Limonite Avenue, and any future Internal Streets adjacent to or within the planning area. Future development design may permit internal access between Planning Area 2 and adjoining Planning Areas.
- 2) Planning Area 2 is located in Zone C of the CLUP for the Chino Airport. Development in this zone will comply with all applicable restrictions and requirements.
- 3) Pedestrian access is by way of sidewalks along roads and internal pathways to individual businesses that provide connections throughout the project as further described in Section 3.5.
- 4) A Ranch at Eastvale Primary Monument shall be provided at the southwest corner of the planning area, as described in Section 3.6.
- 5) A Ranch at Eastvale Secondary Monument shall be provided at the northwest corner of the planning area, as described in Section 3.6.
- 6) A Major Retail/Mixed Use Tenant Monument, may be provided along a minimum of one entry road/driveway accessing Planning Area 2 from Limonite Avenue as described in Section 3.6.
- 7) A Minor Retail/Mixed Use Tenant Monument, may be provided along a minimum of one entry road/driveway accessing Planning Area 2 from Hellman Avenue and along a minimum of one entry road/driveway accessing Planning Area 2 from the industrial collector to the north of Planning Area 2 as described in Section 3.6.
- 8) Please refer to Section 3.0 for specific Design Guidelines and other related design criteria and to Section 4.0 for specific Landscaping Guidelines.
- 9) Please refer Sections 2.2 through 2.5 for Development Plans and Standards that apply site-wide.

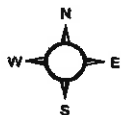
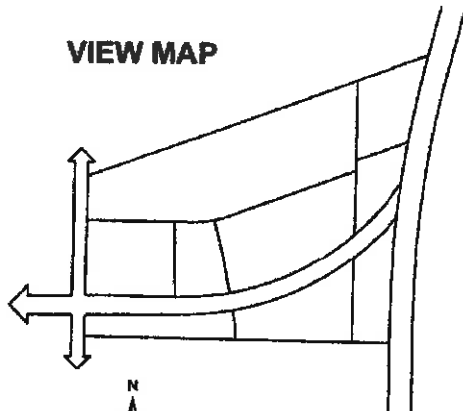
LEGEND

-  THE RANCH AT EASTVALE PRIMARY MONUMENT
-  THE RANCH AT EASTVALE SECONDARY MONUMENT
-  5' MIN. SIDEWALK - PEDESTRIAN CIRCULATION
-  INDUSTRIAL COLLECTOR ENTRIES
-  PLANNING AREA ACCESS
-  MAJOR RETAIL/ MIXED USE TENANT MONUMENT
-  MINOR RETAIL/MIXED USE TENANT MONUMENT

Planning Area 2
MIXED USE
11.6 Acres



VIEW MAP



Not to Scale

Figure 2-12
Planning Area 2

2.7.3 Planning Area 3: Business Park

Planning Area 3 in the west central internal portion of The Ranch at Eastvale Specific Plan north of Limonite Avenue, depicted in **Figure 2-13, Planning Area 3**, provides for the development of approximately 5.6 acres of business park land uses. A maximum 120,000 square feet of buildings can be constructed within Planning Area 3 with building sizes not to exceed 120,000 square feet. The building intensity in Planning Area 3 will not exceed a FAR of 0.60.






Land Use and Development Standards

For permitted land uses and development standards such as setbacks and yard requirements, refer to Table 2-2, Allowable Land Uses and Table 2-3, Development Standards, except as otherwise provided for in the text and exhibits of this Specific Plan.

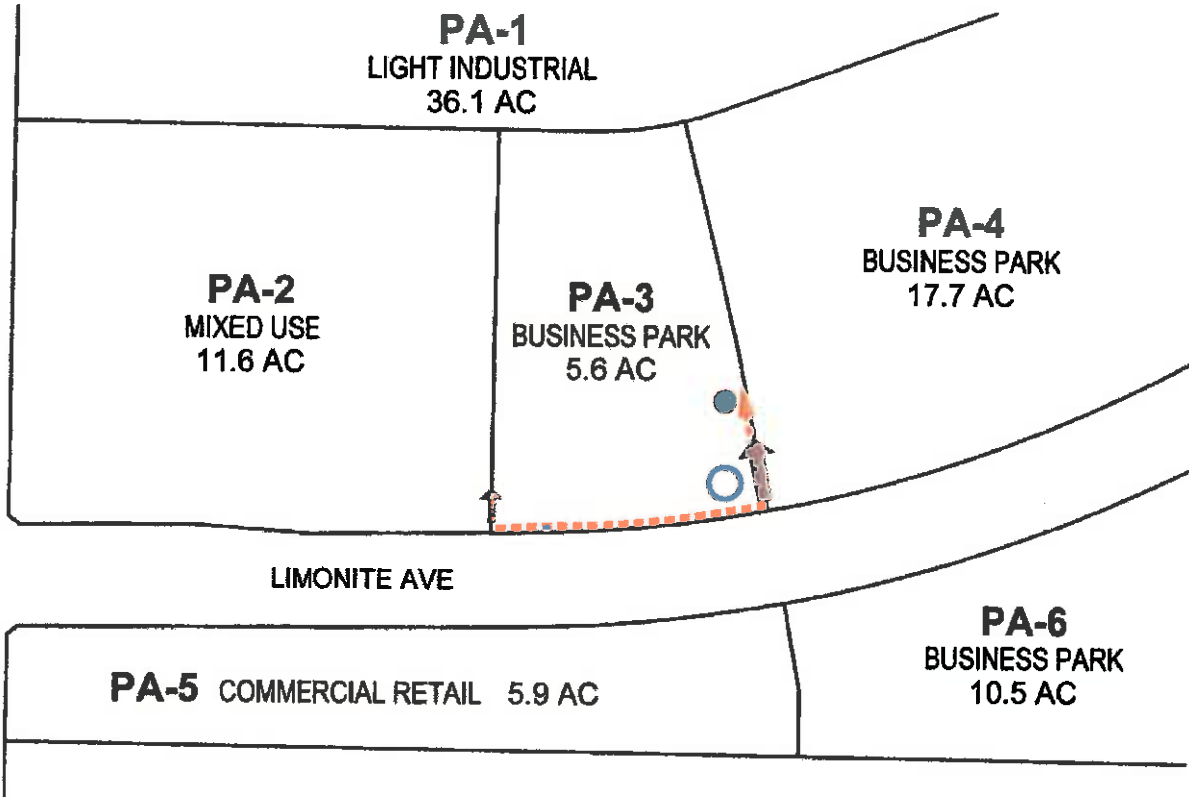
Planning Standards

- 1) Access to Planning Area 3 shall be provided from Limonite Avenue and/or from future Internal Streets. Future development design may permit internal access between Planning Area 3 and adjoining Planning Areas.
- 2) Planning Area 3 is located in Zone C of the CLUP for the Chino Airport. Development in this zone will comply with all applicable restrictions and requirements.
- 3) Pedestrian access is by way of sidewalks along roads and internal pathways to individual businesses that provide connections throughout the project as further described in Section 3.5.
- 4) A Ranch at Eastvale Secondary Monument shall be provided at the southeast corner of the planning area, as described in Section 3.6.
- 5) A Minor Retail/Mixed Use Tenant Monument, may be provided along a minimum of one entry road/driveway accessing Planning Area 3 from the industrial collector east of Planning Area 3 as described in Section 3.6.
- 6) Please refer to Sections 3.0 for specific Design Guidelines and other related design criteria and to Section 4.0 for specific Landscaping Guidelines.
- 7) Please refer to Sections 2.2 through 2.5 for Development Plans and Standards that apply site-wide.

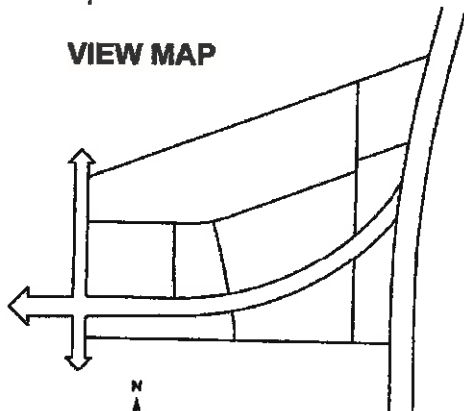
LEGEND

-  THE RANCH AT EASTVALE SECONDARY MONUMENT
-  5' MIN. SIDEWALK - PEDESTRIAN CIRCULATION
-  INDUSTRIAL COLLECTOR ENTRIES
-  PLANNING AREA ACCESS
-  MINOR RETAIL/MIXED USE TENANT MONUMENT

Planning Area 3
BUSINESS PARK
5.6 Acres



VIEW MAP



Not to Scale

Figure 2-13
Planning Area 3

2.7.4 Planning Area 4: Business Park

Planning Area 4 in the internal portion of The Ranch at Eastvale Specific Plan as depicted in **Figure 2-14, Planning Area 4**, provides for the development of approximately 17.7 acres of business park land. A maximum 316,000 square feet of buildings can be constructed with building sizes not to exceed 125,000 square feet within Planning Area 4. The building intensity in Planning Area 4 will not exceed a FAR of 0.60.





Land Use and Development Standards

For permitted land uses and development standards such as setbacks and yard requirements, refer to Table 2-2, Allowable Land Uses and Table 2-3, Development Standards, except as otherwise provided for in the text and exhibits of this Specific Plan.

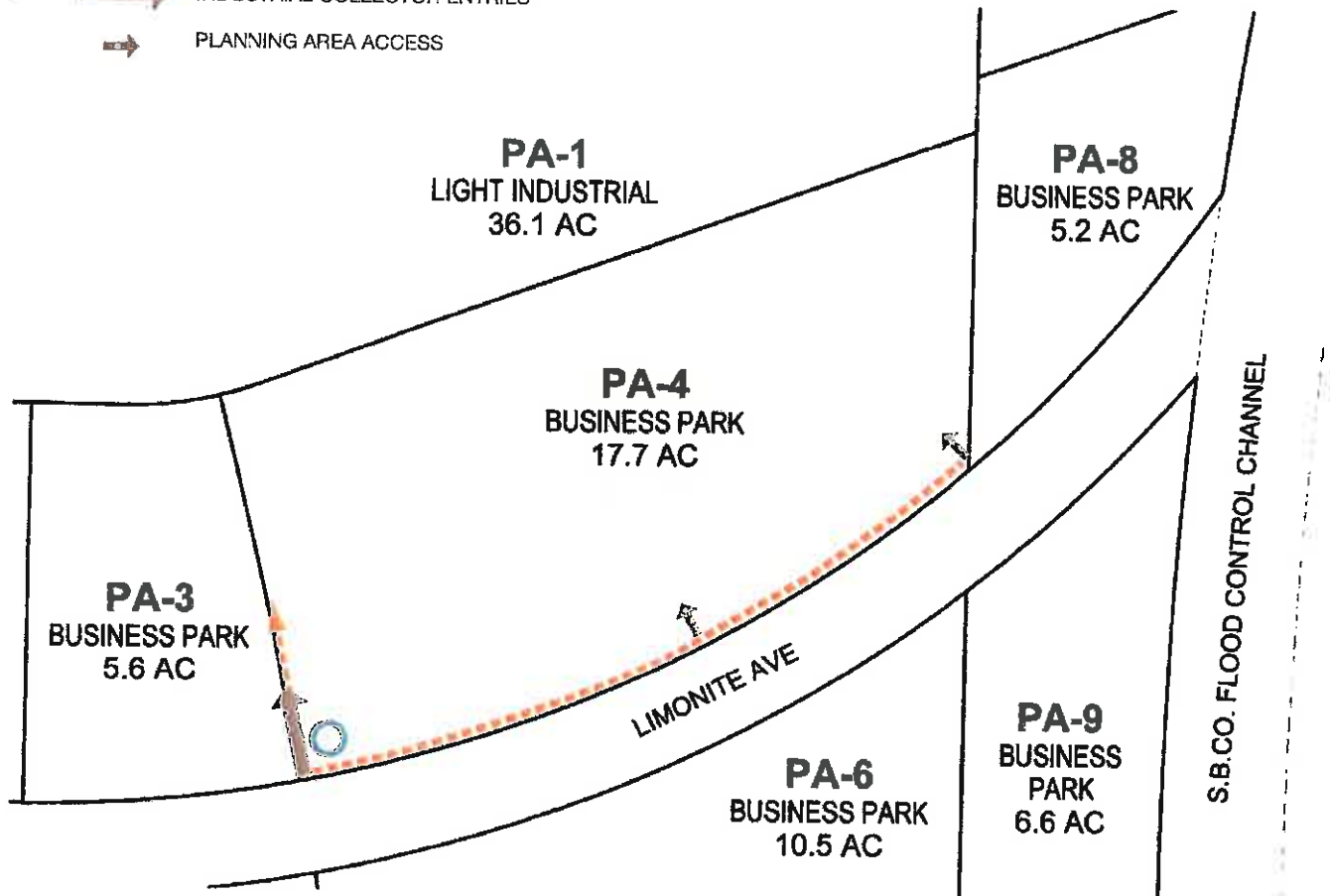
Planning Standards

- 1) Access to Planning Area 4 shall be provided from Limonite Avenue and/or from future Internal Street(s). Future development design may permit internal access between Planning Area 4 and adjoining Planning Areas.
- 2) Planning Area 4 is located in Zone C of the CLUP for the Chino Airport. Development in this zone will comply with all applicable restrictions and requirements.
- 3) Pedestrian access is by way of sidewalks along roads and internal pathways to individual businesses that provide connections throughout the project.
- 4) A Ranch at Eastvale Secondary Monument shall be provided at the southwest corner of the planning area, as described in Section 3.6.
- 5) Please refer to Section 3.0 for specific Design Guidelines and other related design criteria and to Section 4.0 for specific Landscaping Guidelines.
- 6) Please refer to Sections 2.2 through 2.5 for Development Plans and Standards that apply site-wide.

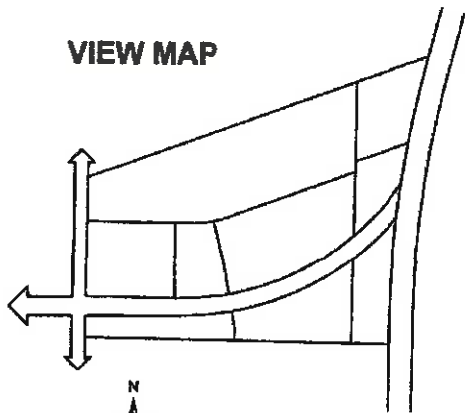
LEGEND

-  THE RANCH AT EASTVALE SECONDARY MONUMENT
-  5' MIN. SIDEWALK - PEDESTRIAN CIRCULATION
-  INDUSTRIAL COLLECTOR ENTRIES
-  PLANNING AREA ACCESS

Planning Area 4
BUSINESS PARK
17.7 Acres



VIEW MAP



Not to Scale

Figure 2-14
Planning Area 4

2.7.5 Planning Area 5: Commercial-Retail

Planning Area 5 in the southwestern portion of site located south of Limonite Avenue and east of Hellman Avenue as depicted in **Figure 2-15, Planning Area 5**, provides for the development of approximately 5.9 acres of commercial-retail land uses. A maximum of 50,000 square feet of buildings can be constructed within Planning Area 5 with no limitation on building size. The building intensity in Planning Area 5 will not exceed a FAR of 0.35.








Land Use and Development Standards

For permitted land uses and development standards such as setbacks and yard requirements, refer to Table 2-2, Allowable Land Uses and Table 2-3, Development Standards, except as otherwise provided for in the text and exhibits of this Specific Plan.

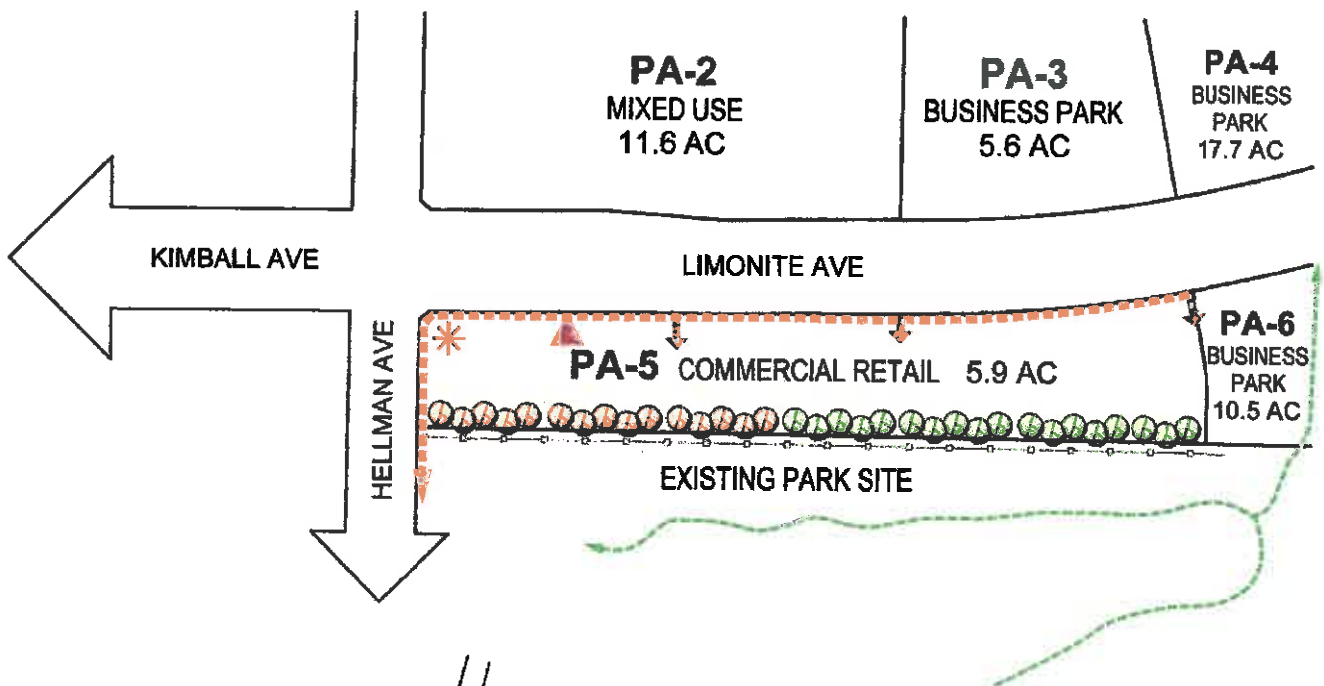
Planning Standards

- 1) Access to Planning Area 5 shall be provided from Limonite Avenue. Future development design may permit access from Hellman Avenue and internal access between Planning Area 5 and adjoining Planning Areas.
- 2) Planning Area 5 is located in Zone D of the CLUP for the Chino Airport. Development in this zone will comply with all applicable restrictions and requirements.
- 3) Pedestrian access is by way of sidewalks along roads and internal pathways to individual businesses that provide connections throughout the project.
- 4) Community of Eastvale Monument shall be provided at the northwest corner of the planning area as described in Section 3.6.
- 5) A Major Retail/Mixed Use Tenant Monument, may be provided along a minimum of one entry road/driveway accessing Planning Area 5 from Limonite Avenue as described in Section 3.6.
- 6) Please refer to Section 3.0 for specific Design Guidelines and other related design criteria and to Section 4.0 for specific Landscaping Guidelines.
- 7) Please refer to Sections 2.2 through 2.5 for Development Plans and Standards that apply site-wide.

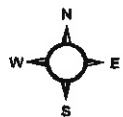
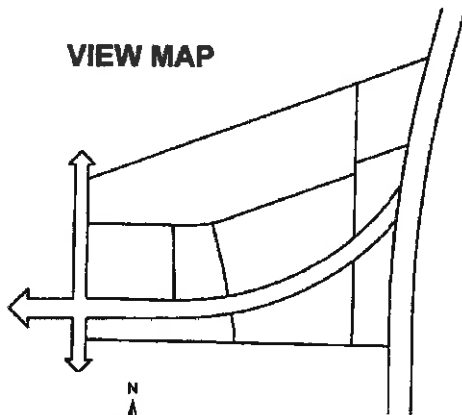
LEGEND

-  THE COMMUNITY OF EASTVALE MONUMENT
-  MAJOR RETAIL/MIXED USE TENANT MONUMENT
-  EXISTING FENCE BY OTHERS
-  TREE EDGE AND SHRUB BUFFER
-  PLANNING AREA ACCESS
-  5' MIN. SIDEWALK - PEDESTRIAN CIRCULATION
-  JCSD MULTI-USE TRAIL (OFF-SITE CLASS 1)

Planning Area 5
COMMERCIAL RETAIL
5.9 Acres



VIEW MAP



Not to Scale

Figure 2-15
Planning Area 5

2.7.6 Planning Area 6: Business Park

Planning Area 6 in the east-central portion of The Ranch at Eastvale Specific Plan along Limonite Avenue as depicted in **Figure 2-16, Planning Area 6**, provides for the development of approximately 10.5 acres of business park land uses. A maximum 88,000 square feet of buildings can be constructed within Planning Area 6 with building sizes not to exceed 40,000 square feet. The building intensity in Planning Area 6 will not exceed a FAR of 0.60.






Land Use and Development Standards

For permitted land uses and development standards such as setbacks and yard requirements, refer to Table 2-2, Allowable Land Uses and Table 2-3, Development Standards, except as otherwise provided for in the text and exhibits of this Specific Plan.

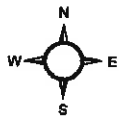
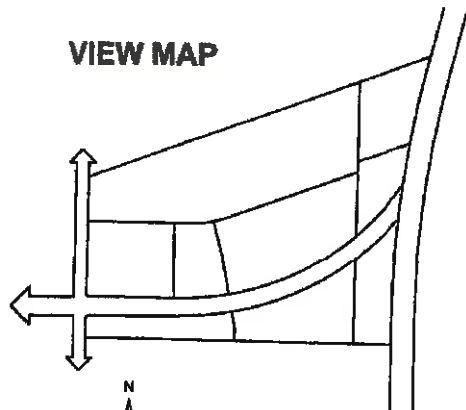
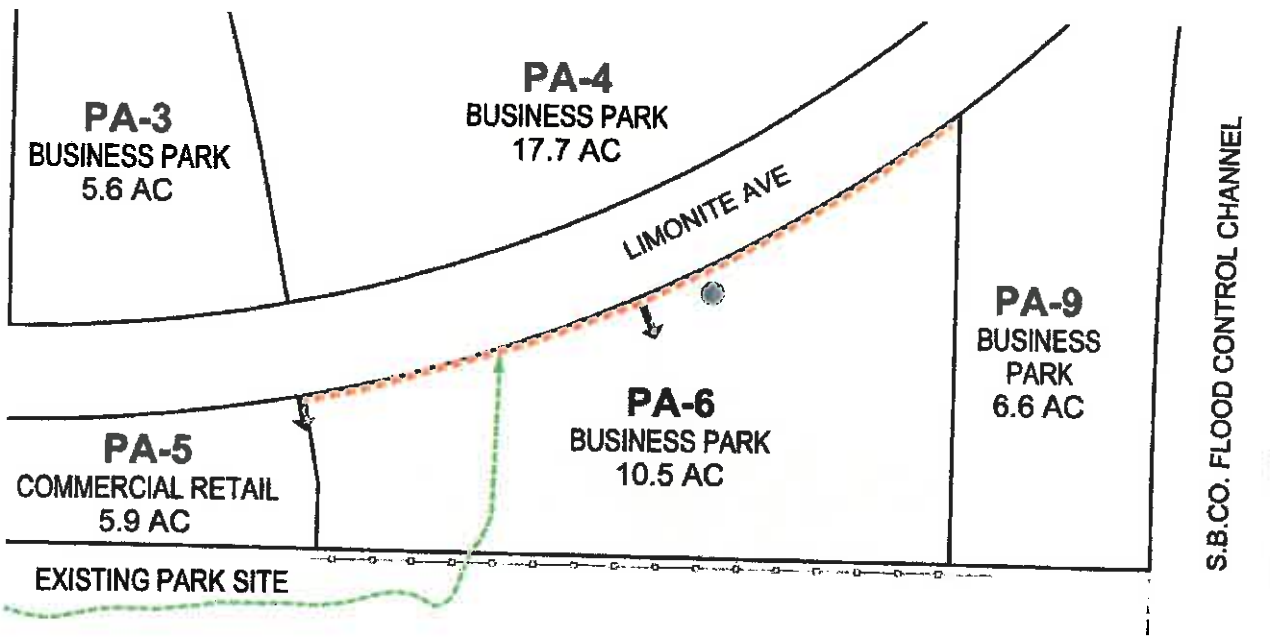
Planning Standards

- 1) Access to Planning Area 6 shall be provided from Limonite Avenue. Future development design may permit internal access between Planning Area 6 and adjoining Planning Areas.
- 2) Planning Area 6 is located in Zones C and D of the CLUP for the Chino Airport. Development in these zones will comply with all applicable restrictions and requirements.
- 3) Pedestrian access is by way of sidewalks along roads and internal pathways to individual businesses that provide connections throughout the project. A 14 foot trail transects Planning Area 6 to provide a regional connection from Limonite to the JSCD trail just south of The Ranch at Eastvale Specific Plan.
- 4) A Minor Retail/Mixed Use Tenant Monument, may be provided along a minimum of one entry road/driveway accessing Planning Area 6 from Limonite Avenue as described in Section 3.6.
- 5) Please refer to Section 3.0 for specific Design Guidelines and other related design criteria and to Section 4.0 for specific Landscaping Guidelines.
- 6) Please refer to Sections 2.2 through 2.5 for Development Plans and Standards that apply site-wide.

LEGEND

-  MINOR RETAIL/MIXED USE TENANT MONUMENT
-  EXISTING FENCE BY OTHERS
-  PLANNING AREA ACCESS
-  5' MIN. SIDEWALK - PEDESTRIAN CIRCULATION
-  JCSD MULTI-USE TRAIL (OFF-SITE CLASS 1)

Planning Area 6
BUSINESS PARK
10.5 Acres



Not to Scale

Figure 2-16
Planning Area 6

2.7.7 Planning Area 7: Light Industrial

Planning Area 7 located in the northwest corner of The Ranch at Eastvale Specific Plan as depicted in **Figure 2-17, Planning Area 7**, provides for the development of approximately 8.4 acres of light industrial land uses. A maximum 156,937 square feet of buildings can be constructed within Planning Area 7. The building intensity in Planning Area 7 will not exceed a FAR of 0.50. Within Planning Area 7, land uses will be limited to agriculture and related uses as interim uses until such time as Map No. 4 of Mira Loma Agricultural Preserve No. 3 has been diminished or disestablished in this planning area and any corresponding Williamson Act contract is no longer in effect for this planning area.







Land Use and Development Standards

For permitted land uses and development standards such as setbacks and yard requirements, refer to Table 2-2, Allowable Land Uses and Table 2-3, Development Standards, except as otherwise provided for in the text and exhibits of this Specific Plan.

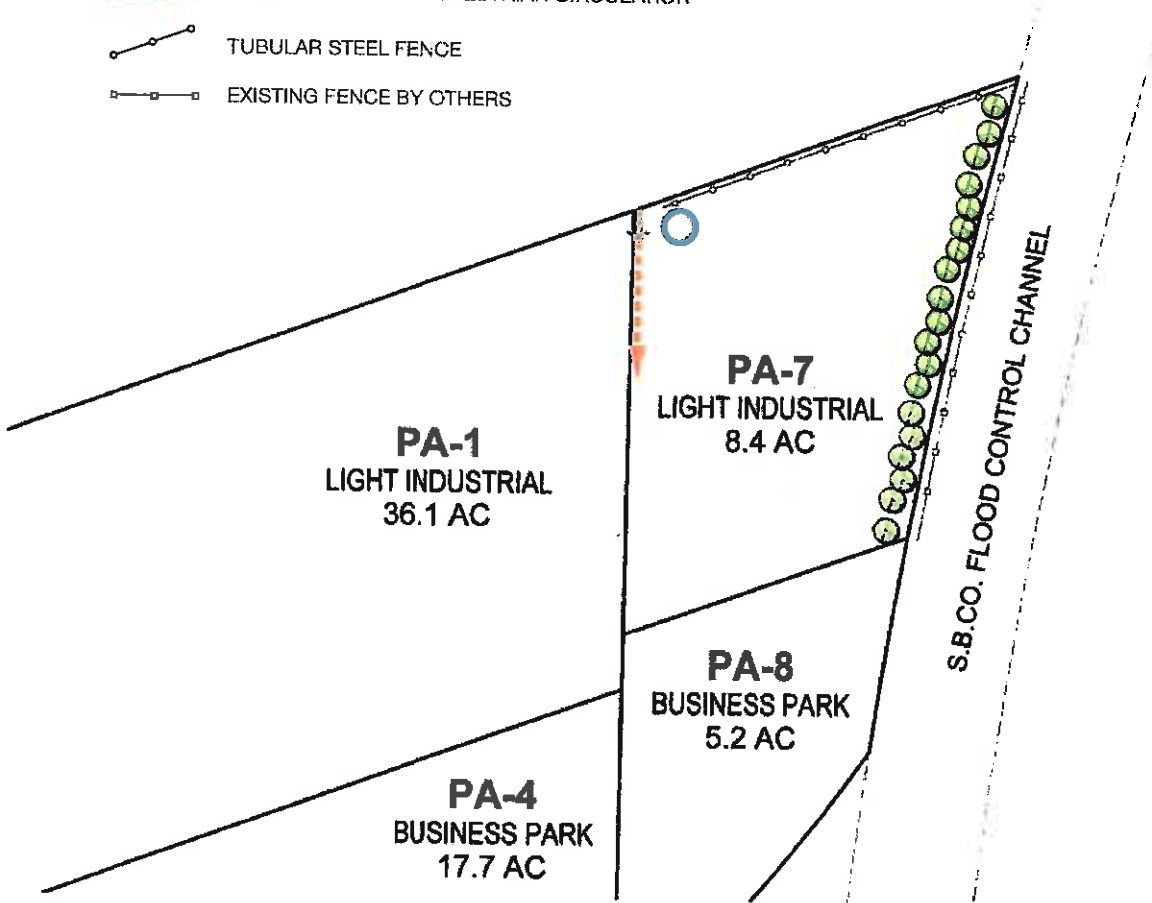
Planning Standards

- 1) Access to Planning Area 7 shall be provided from future Internal Streets. Future development design may permit internal access between Planning Area 7 and adjoining Planning Areas.
- 2) Planning Area 7 is located in Zone C of the CLUP for the Chino Airport.
- 3) The Chino Airport extended runway centerline crosses into the northern portion of Planning Area 7. As such, all structures shall be located a maximum distance from the extended runway centerline.
- 4) Development in this zone will comply with all applicable restrictions and requirements.
- 5) Pedestrian access is by way of sidewalks along roads and internal pathways to individual businesses that provide connections throughout the project.
- 6) Signage to be provided as depicted in Figure 3-1, Monument Plan and as described in Section 3.6.
- 7) Please refer to Section 3.0 for specific Design Guidelines and other related design criteria and to Section 4.0 for specific Landscaping Guidelines.
- 8) Please refer to Sections 2.2 through 2.5 for Development Plans and Standards that apply site-wide.

LEGEND

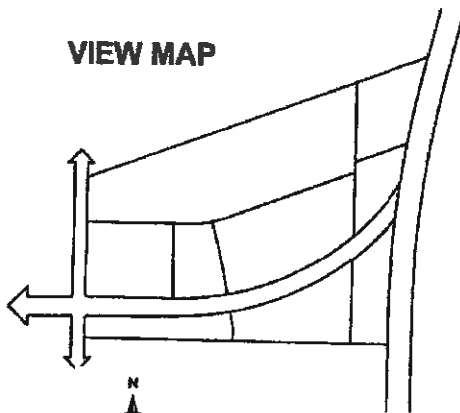
-  THE RANCH AT EASTVALE SECONDARY MONUMENT
-  TREE EDGE AND SHRUB BUFFER
-  PLANNING AREA ACCESS
-  5' MIN. SIDEWALK - PEDESTRIAN CIRCULATION
-  TUBULAR STEEL FENCE
-  EXISTING FENCE BY OTHERS

Planning Area 7
LIGHT INDUSTRIAL
8.4 Acres



S.B.CO. FLOOD CONTROL CHANNEL

VIEW MAP



Not to Scale

Figure 2-17
Planning Area 7

2.7.8 Planning Area 8: Business Park

Planning Area 8 located at the center of the eastern project boundary and on the north side of Limonite Avenue as depicted in **Figure 2-18, Planning Area 8**, provides for the development of approximately 5.2 acres of business park land uses. A maximum 122,216 square feet of buildings can be constructed within Planning Area 8. The building intensity in Planning Area 8 will not exceed a FAR of 0.60. Within Planning Area 8, land uses will be limited to agriculture and related uses as interim uses until such time as Map No. 4 of Mira Loma Agricultural Preserve No. 3 has been diminished or disestablished in this planning area and any corresponding Williamson Act contract is no longer in effect for this planning area.






Land Use and Development Standards

For permitted land uses and development standards such as setbacks and yard requirements, refer to Table 2-2, Allowable Land Uses and Table 2-3, Development Standards, except as otherwise provided for in the text and exhibits of this Specific Plan.

Planning Standards

- 1) Access to Planning Area 8 can be provided from Limonite Avenue or future internal streets. Future development design may permit internal access between Planning Area 8 and adjoining Planning Areas.
- 2) Planning Area 8 is located in Zone C of the CLUP for the Chino Airport. Development in this zone will comply with all applicable restrictions and requirements.
- 3) Pedestrian access is by way of sidewalks along roads and internal pathways to individual businesses that provide connections throughout the project.
- 4) Signage to be provided as depicted in Figure 3-1, Monument Plan and as described in Section 3.6.
- 5) Please refer to Section 3.0 for specific Design Guidelines and other related design criteria and to Section 4.0 for specific Landscaping Guidelines.
- 6) Please refer to Sections 2.2 through 2.5 for Development Plans and Standards that apply site-wide.

LEGEND

-  TENANT SIGNAGE
-  TREE EDGE AND SHRUB BUFFER
-  PLANNING AREA ACCESS
-  5' MIN. SIDEWALK - PEDESTRIAN CIRCULATION
-  EXISTING FENCE BY OTHERS

Planning Area 8
BUSINESS PARK
5.2 Acres

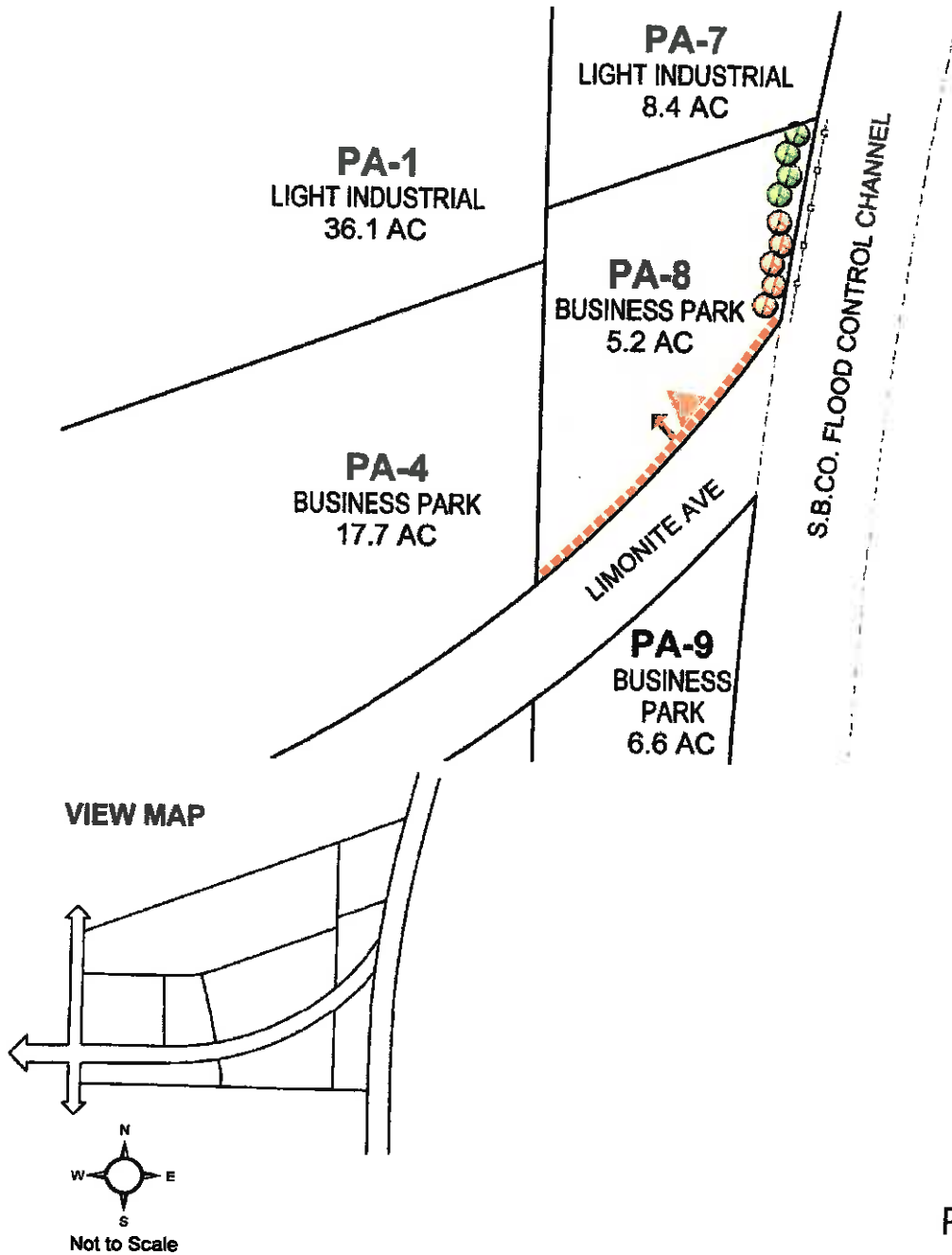


Figure 2-18
Planning Area 8

2.7.9 Planning Area 9: Business Park

Planning Area 9 located between the southern project boundary and the south side of Limonite Avenue as depicted in **Figure 2-19, Planning Area 9**, provides for the development of approximately 6.6 acres of business park land uses. A maximum 155,121 square feet of buildings can be constructed within Planning Area 9. The building intensity in Planning Area 9 will not exceed a FAR of 0.60. Within Planning Area, land uses will be limited to agriculture and related uses as interim uses until such time as Map No. 4 of Mira Loma Agricultural Preserve No. 3 has been diminished or disestablished in this planning area and any corresponding Williamson Act contract is no longer in effect for this planning area.





Land Use and Development Standards

For permitted land uses and development standards such as setbacks and yard requirements, refer to Table 2-2, Allowable Land Uses and Table 2-3, Development Standards, except as otherwise provided for in the text and exhibits of this Specific Plan.

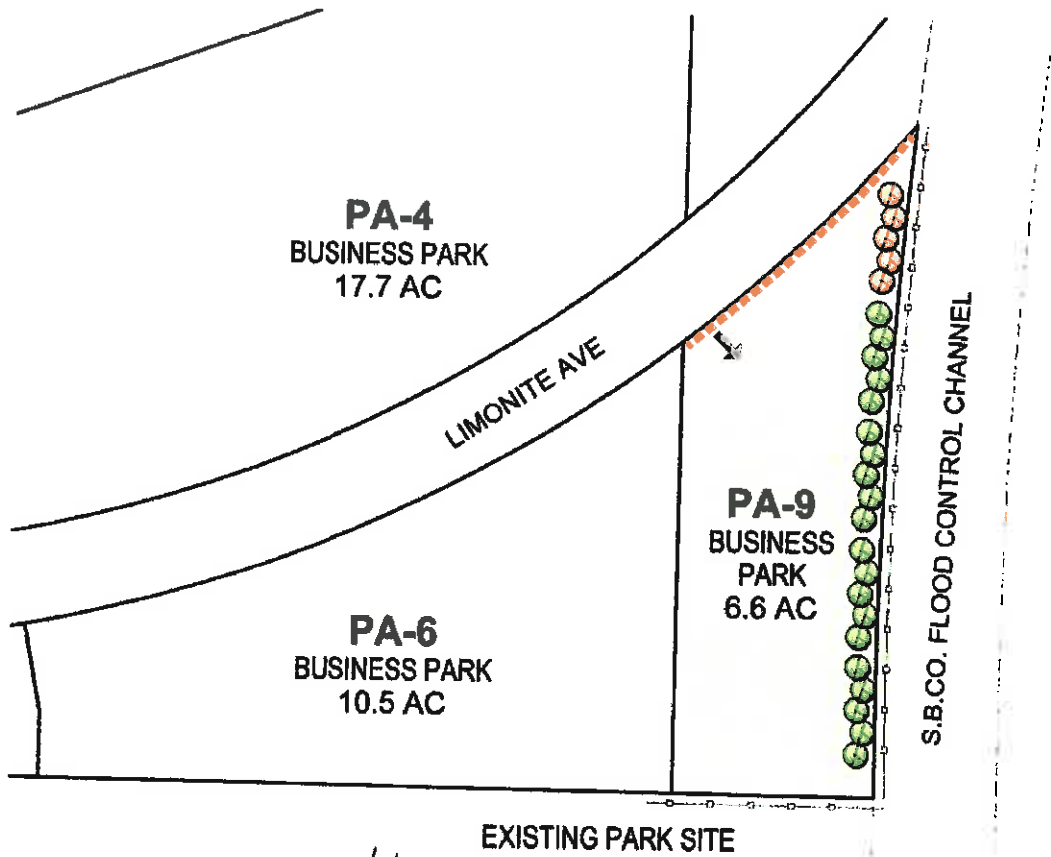
Planning Standards

- 1) Access to Planning Area 9 shall be provided from Limonite Avenue. Future development design may permit internal access between Planning Area 9 and adjoining Planning Areas.
- 2) Planning Area 9 is located in Zones C and D of the CLUP for the Chino Airport. Development in these zones will comply with all applicable restrictions and requirements.
- 3) Pedestrian access is by way of sidewalks along roads and internal pathways to individual businesses that provide connections throughout the project.
- 4) Signage to be provided as depicted in Figure 3-1, Monument Plan and as described in Section 3.6.
- 5) Please refer to Section 3.0 for specific Design Guidelines and other related design criteria and to Section 4.0 for specific Landscaping Guidelines.
- 6) Please refer to Sections 2.2 through 2.5 for Development Plans and Standards that apply site-wide.

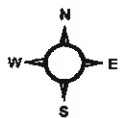
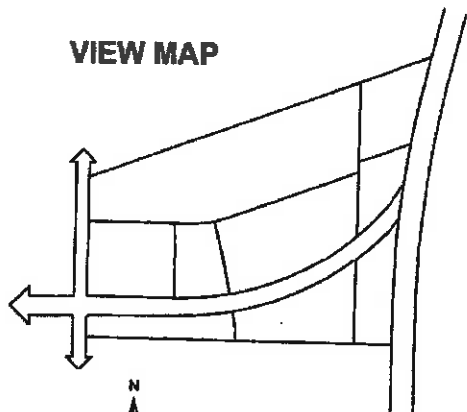
LEGEND

-  TREE EDGE AND SHRUB BUFFER
-  PLANNING AREA ACCESS
-  5' MIN. SIDEWALK - PEDESTRIAN CIRCULATION
-  EXISTING FENCE BY OTHERS

Planning Area 9
BUSINESS PARK
6.6 Acres



VIEW MAP



Not to Scale

Figure 2-19
Planning Area 9

NOTICE OF PUBLIC HEARING

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Thursday, from 8:00 a.m. to 5:00 p.m., except Monday, September 7 (Labor Day).

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon St., 1st Floor Hearing Room
Riverside, California

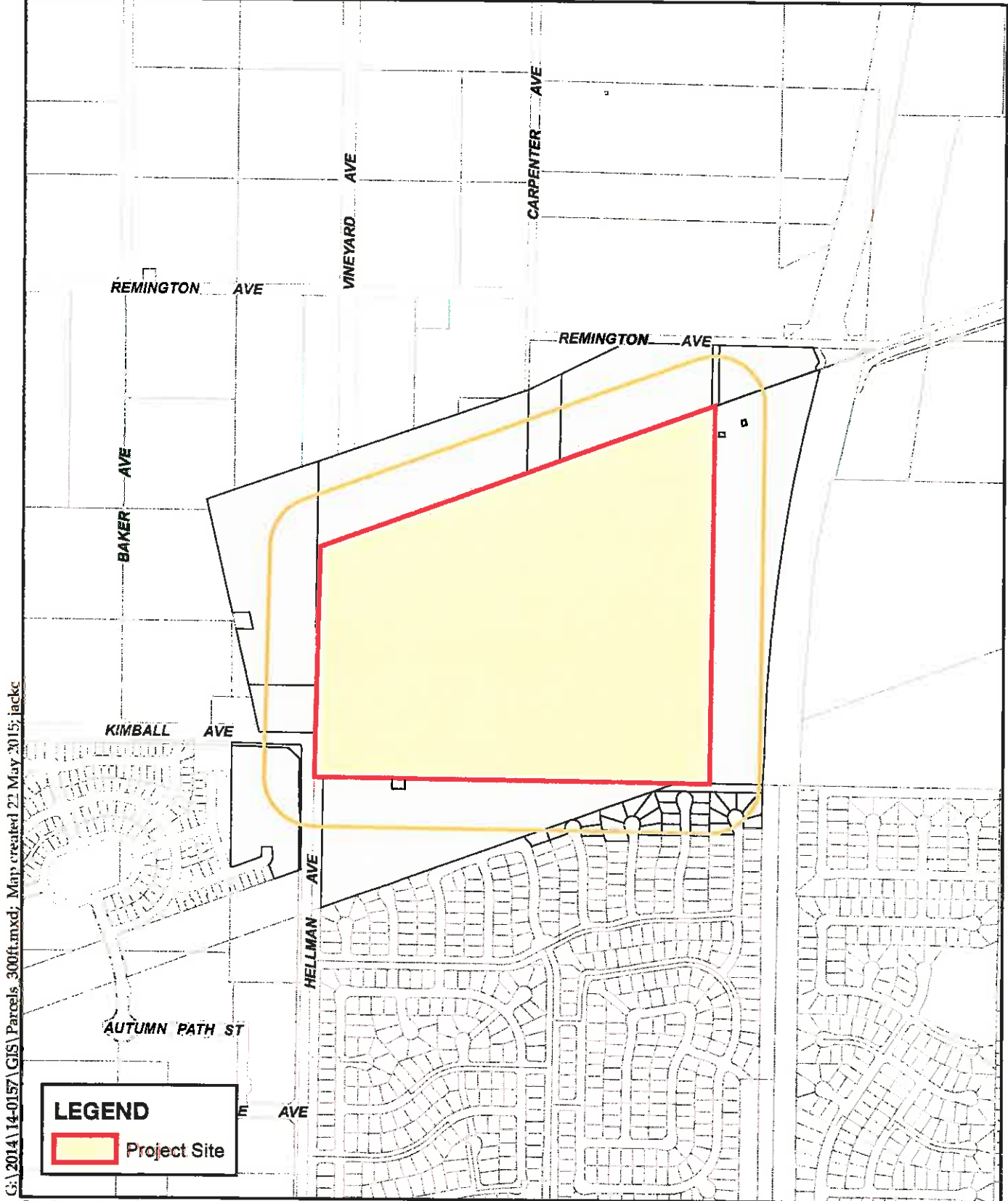
DATE OF HEARING: September 10, 2015

TIME OF HEARING: 9:00 A.M.

CASE DESCRIPTION:

ZAP1021CH15 – Moons Family Trust (Representative: Summit Development Corporation, Bryan Bentrott and Taylor Gerry) – City of Eastvale Case No. 15-0783 (Specific Plan Amendment [SPA], Tentative Parcel Map [TPM 36787], and Major Development Plan [MDP]. The SPA proposes an amendment to the Ranch at Eastvale Specific Plan located southerly of the San Bernardino County line, easterly of Hellman Avenue, and westerly of Cucamonga Creek Channel in the City of Eastvale to change the land use designation of Planning Area 2 from Commercial/Retail to Mixed Use, modify the boundaries between, and acreages of, the Planning Areas, and modify the permitted uses within each Planning Area. Limonite Avenue will ultimately be extended through this site to connect to Kimball Avenue in the City of Chino. The TPM proposes to subdivide 82.16 net acres (97.98 gross acres) into 15 parcels. The MDP proposes to develop a total of 940,000 square feet of industrial space within 6 shell buildings on 50.09 net acres (56.03 gross acres) (Lots 1-7 of the TPM). Size of each building ranges between 50,000 square feet to 300,000 square feet with anticipated uses primarily consisting of warehousing with supporting offices. **FURTHER CONSIDERATION: This project is being reconsidered due to changes in locations of required open areas.** (Compatibility Zones B1 and C of Chino Airport Influence Area).

FURTHER INFORMATION: Contact Russell Brady at (951) 955-0549 or John Guerin at (951) 955-0982. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Ms. Kanika Kith of the City of Eastvale Planning Department, at (951) 703-4460.



G:\2014\14-0157\GIS\Parcels_300ft.mxd; Map created 22 May 2015; jackc

Source: Riverside Co. GIS, April, 2015
 San Bernardino Co GIS, March, 2015

Parcels Within 300 ft. of Project
 Eastvale Ranch



APPLICATION FOR MAJOR LAND USE ACTION REVIEW
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

ALUC Identification No.

ZAP1021CH15

PROJECT PROPONENT (TO BE COMPLETED BY APPLICANT)

Date of Application May 26, 2015
 Property Owner Moons Family Trust
 Mailing Address 4974 Avenue 272 Phone Number (559) 240-1508
Visalia, CA 93277
Bryan Bentrutt / Taylor Gerry (949) 655-8226
c/o Summit Development Corporation
 Agent (if any) 450 Newport Center Drive, Suite 625 Phone Number _____
 Mailing Address Newport Beach, CA 92660

PROJECT LOCATION (TO BE COMPLETED BY APPLICANT)

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address NEC of Limonite Avenue & Hellman Avenue
Eastvale, CA
 Assessor's Parcel No. 144-010-008-0 & 144-010-013-4 Parcel Size 97 gross acres
 Subdivision Name The Ranch at Eastvale
 Lot Number _____ Zoning Classification Specific Plan

PROJECT DESCRIPTION (TO BE COMPLETED BY APPLICANT)

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe) Vacant land subject to Specific Plan No. 358 called "The Ranch at Eastvale" approved by the County of Riverside May 25, 2010.

Proposed Land Use (describe) Major Development Plan approval for seven (7) industrial buildings ranging in seize from 50,000 to 300,000 S.F. pursuant to a S.P. Amendment.

For Residential Uses Number of Parcels or Units on Site (exclude secondary units) _____
 For Other Land Uses Hours of Use 8-12 hours, 5 days per week
 (See Appendix C) Number of People on Site Maximum Number 2,026 (Zone B-1 & Zone C)
 Method of Calculation Applied maximum overall density of people who would be in the building per code.

Height Data Height above Ground or Tallest Object (including antennas and trees) 43 ft. max ft.
 Highest Elevation (above sea level) of Any Object or Terrain on Site 682 ft. max ft.

Flight Hazards Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight? Yes
 No
 If yes, describe _____

REFERRING AGENCY (APPLICANT OR JURISDICTION TO COMPLETE)

Date Received	May 6, 2015	Type of Project
Agency Name	City of Eastvale	<input type="checkbox"/> General Plan Amendment
	Kanika Kith	<input checked="" type="checkbox"/> Zoning Amendment or Variance (SPA)
Staff Contact	(951) 703-4460	<input type="checkbox"/> Subdivision Approval
Phone Number	15-0783	<input type="checkbox"/> Use Permit
Agency's Project No.		<input type="checkbox"/> Public Facility
		<input checked="" type="checkbox"/> Other <u>Development Plan Approval</u>

A. **NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. **SUBMISSION PACKAGE:**

ALUC REVIEW

- 1. Completed Application Form
- 1. Project Site Plan – Folded (8-1/2 x 14 max.)
- 1. Elevations of Buildings - Folded
- 1 Each . 8 ½ x 11 reduced copy of the above
- 1. 8 ½ x 11 reduced copy showing project in relationship to airport.
- 1 Set . Floor plans for non-residential projects
- 4 Sets. . Gummed address labels of the Owner and representative (*See Proponent*).
- 1 Set. . Gummed address labels of all property owners within a 300' radius of the project site. If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10), with ALUC return address.
- 4 Sets. . Gummed address labels of the referring agency (City or County).
- 1. Check for Fee (See Item "C" below)

STAFF REVIEW (Consult with ALUC staff planner as to whether project qualifies)

- 1. Completed Application Form
- 1. Project Site Plans – Folded (8-1/2 x 14 max.)
- 1. Elevations of Buildings - Folded
- 1. 8 ½ x 11 Vicinity Map
- 1 Set . Gummed address labels of the Owner and representative (*See Proponent*).
- 1 Set . Gummed address labels of the referring agency.
- 1. Check for review—See Below



**SUMMIT DEVELOPMENT
CORPORATION**

May 26, 2015

John Guerin
Riverside County Airport Land Use Commission
Riverside County Administrative Center
4080 Lemon Street, 14th Floor
Riverside, CA 92501

Re: ALUC Submission Package for the Ranch at Eastvale

Dear John:

I am pleased to present our completed application and site plan for ALUC review prior to our formal submission on or before May 27, 2015. By way of background, ALUC issued the attached conditions of approval dated May 14, 2007. At that time, the ALUC conditions governed a "blob" land use plan for several land parcels which comprised an approximately 119 gross acres Specific Plan known as "The Ranch at Eastvale". This Specific Plan was approved by the County of Riverside as of May 25, 2010.

The parcels covered by the Specific Plan include:

- Moons site (approx. 98 gross acres)
APN: 144-010-008-0 & 144-010-013-4
- Rodriguez site (approx. 20 gross acres)
APN: 144-010-009-1

At this time, we are submitting a Major Development Plan, which will provide site plan approvals for Buildings 1-7 as shown on the site plan (i.e. Phase I). All of the Phase I buildings are located on the Moons site.

We call the Phase I buildings and the related master plan our "Submission Plan". We met with Eric Norris and Kanika Kith on May 6, 2015 and they agreed we could submit the SAME Submission Plan to ALUC staff in order to meet the May 27th submission date for the July 9th ALUC meeting.

A few highlights from the Submission Plan:

- Buildings 1-7 are located in two zones which are known as B-1 and C. The B-1 zone requires a 30% open space component and the C zone requires a 20% open space component.
- We have met the open space requirements for each Building on the site plan and we have taken extra care to make sure our landscape plan complies with ALUC standards. We have also located street lights outside of the open space quadrants noted on the Submission Plan.

- The Phase I open space areas are highlighted in yellow and we are taking the extra step of showing you how we intend to meet our open space requirements for the “Western Quadrant” by noting the open space areas in blue. We call this Western Quadrant “Phase II.”
- The Phase I buildings will more than likely operate according to “normal” business hours, but we cannot preclude a 24/7 operation.
- We have attached a chart to explain the maximum employee densities for the Phase I buildings.
- “Phase III” is generally described as the land south of Limonite and this area requires a 10% open space requirement. We do not anticipate having an issue in meeting this requirement when we are ready to submit a specific site plan for this area.
- As suggested, we will be submitting the completed FAA 7460 forms required for each Phase I building in order to facilitate obtaining a Conditional Approval from RCALUC.

John, your previous advice and counsel helped in preparing our Submission Plan. Our previous work gave the city confidence to endorse moving ahead with you now so that we might keep our project on track for approval at the ALUC meeting in July. Thank you for considering a meeting time with Bill and Taylor so we can make sure we have covered every possible detail on our Submission Plan.

Sincerely,



Bryan Bentrutt

Principal
Summit Development Corporation

Direct: 949.655.8226

Cell: 949.395.2488

Email: bryan@sumdevco.com

**RIVERSIDE COUNTY LAND USE COMMISSION
COMPLIANCE METHODOLOGY**

The Ranch at Eastvale – Phase 1

May 27, 2015

Gentleman,

On May 10, 2007 the Riverside ALUC, in reviewing an application for development review (File No. ZAP1004CH07) found the site we proposed to be developed consistent with the then approved 1984 Riverside County Airport Land Use Plan, as applied to the Airport Influence area of Chino Airport, subject to a series of conditions. Summit Development Corp. now plans to develop portions of the site reviewed in 2007 in two or more phases.

On May 6, 2015 Summit Development Corp. submitted revised plans and specifications to the City of Eastvale for Master Development Plan Review of Phase 1 of the 98 acre Ranch at Eastvale. The **Phase 1** development consists of seven industrial warehouse buildings totaling 945,000 SF located on 56.6 gross acres, along with major roads totaling 12.3 acres. **Phase 2** of The Ranch at Eastvale consists of 29.1 acres of commercial/business park property which will undergo Plan Review at a future date.

The Open Space Zone Diagram dated 05/06/2015 prepared by MacDavid Aubor Architects, and Landing Open Space Area plans prepared by Albert A. Webb Associates Civil Engineers are the basis of the following compliance summary:

- **Open Land:** In addition to showing all of our building envelopes, we have identified in yellow on the Zone Diagram all of the **Phase 1** open land areas comprised of roads and automobile parking lots which are free of structures, other major obstacles such as large trees or poles, and overhead wires. All of these areas have a minimum of 75 feet by 300 ft. Identified in blue all of the open land areas in Zone C being developed with **Phase 1** which we are electing to allocate to future **Phase 2** development in Zone C. The open space summary on the Zone Diagram details our compliance with minimum open space requirements of Zone B1 (30%), and Zone C (20%).
- **Limitations on Clustering:** We have approached the clustering of our **Phase 1** buildings so that they are each separated by qualified open space parking lot areas. Further we have positioned the building office areas to be outside of the most restrictive B1 Zone as an additional safety feature. Please refer to Exhibit A attached, which details calculations of the Maximum Single Acre Intensity for each of our buildings. Note that building #3 & #4 will require an intensity bonus. Exhibit B attached, details the Average Zone Density people contributions for each of our buildings. We are in compliance with the maximum density of Zone B1 (40 / AC), and Zone C (75 / AC).
- **Risk Reduction Measures:** Since actual landing frequency measurements at the Chino Airport does not indicate takeoff and landing frequency of large aircraft having a maximum certificated takeoff weight of more than 12,500 pounds (based on Chino Airport master plan data, less than two percent of the traffic overflying the site will be large aircraft), we are eligible for credit for

the following allowed building design features which would enable an Intensity Bonus for Buildings #3 & #4:

- Using concrete walls
- Enhancing the fire sprinkler system (see Allen Automatic Fire Sprinkler letter)
- Limiting buildings to a single story
- Increasing the number of emergency exits (see McDavid Aubort letter)



Allan Automatic Sprinkler of So. Cal.

Fire Protection Contractor Since 1956

A Division of Shambaugh & Son, L.P.



An EMCOR Company

3233 Enterprise Street
Brea, CA 92821

Phone: 714.993.9500
Fax: 714.993.5708

License #C-16 998265

May 21, 2015

Summit Development Corp
450 Newport Center Drive Suite 626
Newport Beach, CA 92660-7610

Attn: Taylor Gerry

Re: The Ranch at Eastvale
Eastvale, CA
Allan's Project No. 275706

Subject: Fire Protection

Dear Taylor:

Following up on our telephone conversation, we have reviewed the drawings of the proposed buildings at The Ranch at Eastvale.

The fire sprinkler design will be Early Suppression Fast Response (ESFR) sprinklers, which exceeds the requirements of the California Fire Code and NFPA 13.

This is a suppression technology as opposed to conventional control mode technology, and is a significant upgrade.

If you have any questions, please contact our office.

Sincerely,

Shambaugh & Son, L.P.
Allan Automatic Sprinkler Of So Cal, a Division of

Ronald J Stephens

RONALD J. STEPHENS



May 20, 2015

Mr. Taylor Gerry
Project Coordinator

Re: the ranch, Eastvale, CA. (Required vs. Provided Fire Access doors for Buildings 3 & 4)

Hi Taylor,

MAA Architects has calculated the required fire access doors vs. the provided preliminary location of the fire access doors.

Building #3: Required fire access doors= 15 doors
 Provided fire access doors= 19 doors

Building #4: Required fire access doors= 24 doors
 Provided fire access doors= 30 doors

If you have any questions, I can be reached at (949) 305.2200 x224

Sincerely,

A handwritten signature in black ink, appearing to read 'Dan MacDavid', written over a faint, illegible background.

Dan MacDavid,
Principal

MacDavid Aubort and Associates, Inc.

O:\administration_eastvale_taylorgerry_fireaccessdoors_52015.doc

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.1

HEARING DATE: September 10, 2015

CASE NUMBER: ZAP1138MA15 – RC Jenson, Inc. (Representative: Broeske Architects and Associates)

APPROVING JURISDICTION: City of Riverside

JURISDICTION CASE NO: None as yet.

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends a finding of CONSISTENCY, subject to the conditions included herein.

PROJECT DESCRIPTION: The applicant proposes to add 2,500 square feet of first floor office space and 2,730 square feet of second floor office space within an existing industrial warehouse building on a 3.44-acre parcel, increasing the net building area to 73,652 square feet.

PROJECT LOCATION: The site is located at the southeasterly corner of Eastridge Avenue and, Lance Drive, within the City of Riverside, approximately 15,500 feet northwesterly of the northerly end of Runway 14-32 at March Air Reserve Base.

LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

- a. Airport Influence Area: March Air Reserve Base
- b. Land Use Policy: Zone C1
- c. Noise Levels: 60-65 CNEL from aircraft

BACKGROUND:

Non-Residential Average Land Use Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zone C1. Average intensity in Compatibility Zone C1 is limited to 100 people per acre. The site is 3.44 acres in area.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan,

and March Air Reserve Base/Inland Port Airport Compatibility Plan Policy 2.4, the following rates were used to calculate the occupancy for the proposed expanded building:

- Warehouse –1 person per 500 square feet
- Office – 1 person per 200 square feet (with 50% reduction)

Based on the floor plan provided, the building would include 7,725 square feet of first floor office area, 5,807 square feet of second floor office area, and 63,283 square feet as warehouse for a total occupancy of 195 people. Given a site area of 3.44 acres, this results in an average intensity of 57 people per acre, which is compatible with the Zone C1 average acre criterion of 100.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per standard vehicle and 1.0 persons per truck trailer parking/dock space in the absence of more precise data). Based on the number of standard parking spaces provided of 92 and truck docks of 14, the total occupancy would be estimated at 152 people. This results in an average intensity of 44 people per acre, which is also compatible with the Zone C1 average acre criterion of 100.

Non-Residential Single-Acre Land Use Intensity: Compatibility Zone C1 limits maximum single-acre intensity to 250 people. There are no risk-reduction design bonuses available, as March Air Reserve Base/Inland Port Airport is primarily utilized by large aircraft weighing more than 12,500 pounds.

Based on the floor plan provided and the occupancies as previously noted, the maximum single-acre area would consist of all of the 7,725 square feet of first floor office area, 5,807 square feet of second floor office area, and 35,835 square feet of warehouse area. This would result in a single-acre occupancy of 140, which would be consistent with the single-acre criterion of 250.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zone C1.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being in an area between 60-65 CNEL range from aircraft noise. As an industrial use not sensitive to noise (and considering typical anticipated building construction noise attenuation of approximately 20 dBA), the warehouse area would not require special measures to mitigate aircraft-generated noise. However, a condition is included to ensure adequate noise attenuation is provided for office uses.

Part 77: The applicant does not propose any exterior improvements that would increase the height of the building. Therefore, review by the FAA Obstruction Evaluation Service is not required for height/elevation reasons.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP

require open area specifically.

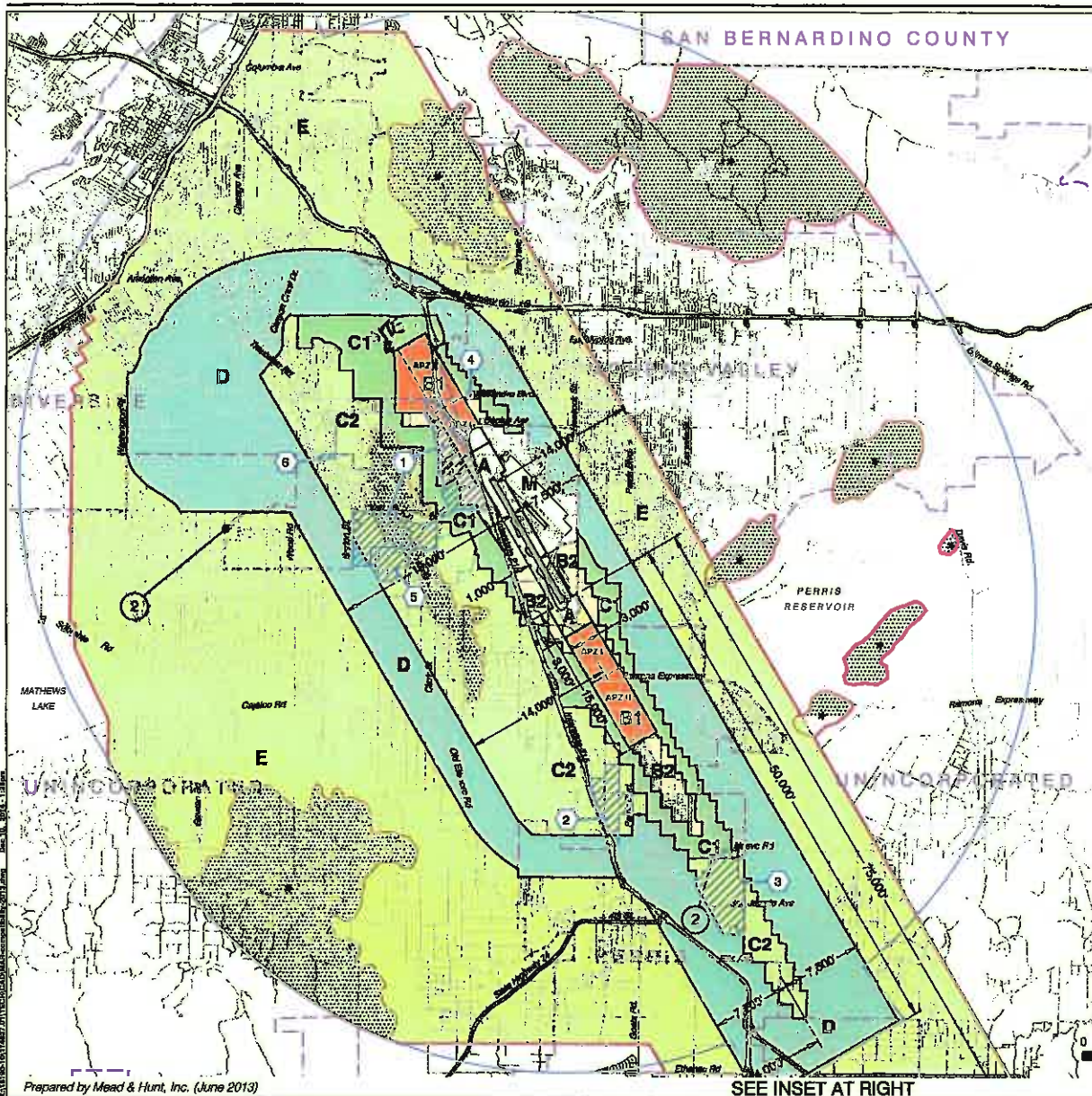
CONDITIONS:

1. Any new outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, congregate care facilities, hotels/motels, places of assembly, noise sensitive outdoor nonresidential uses and hazards to flight.
3. The attached notice shall be given to all prospective purchasers of the property and tenants of the building, and shall be recorded as a deed notice.
4. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.

5. Office space must have sound attenuation features sufficient to reduce interior noise levels from exterior aviation-related sources to no more than CNEL 45 dB. The City of Riverside shall require an acoustical study to ensure compliance with this requirement.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



LEGEND

Compatibility Zones

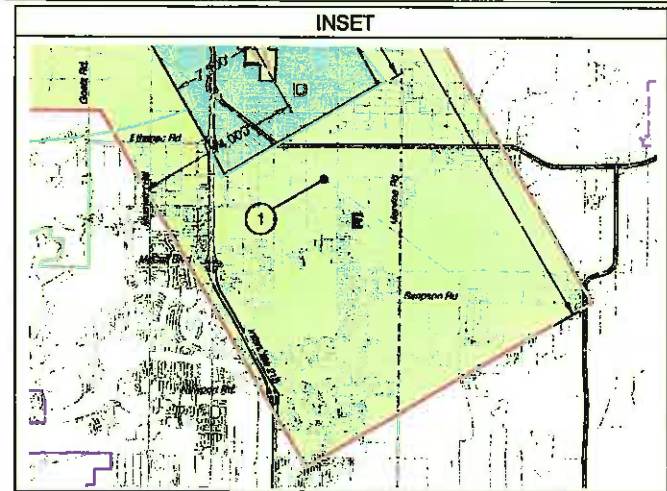
- █ Airport Influence Area Boundary
- █ Zone A
- █ Zone B1
- █ Zone B2
- █ Zone C1
- █ Zone C2
- █ Zone D
- █ Zone E
- █ Zone M
- █ High Terrain Zone
- █ FAR Part 77 Military Outer Horizontal Surface Limits
- █ FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- ▭ Site-Specific Exceptions (existing local agency commitments to development projects)

- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



**Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)**

Note:
All dimensions are measured from runway ends and centerlines.




Base map source: County of Riverside 2013

Prepared by Mead & Hunt, Inc. (June 2013)

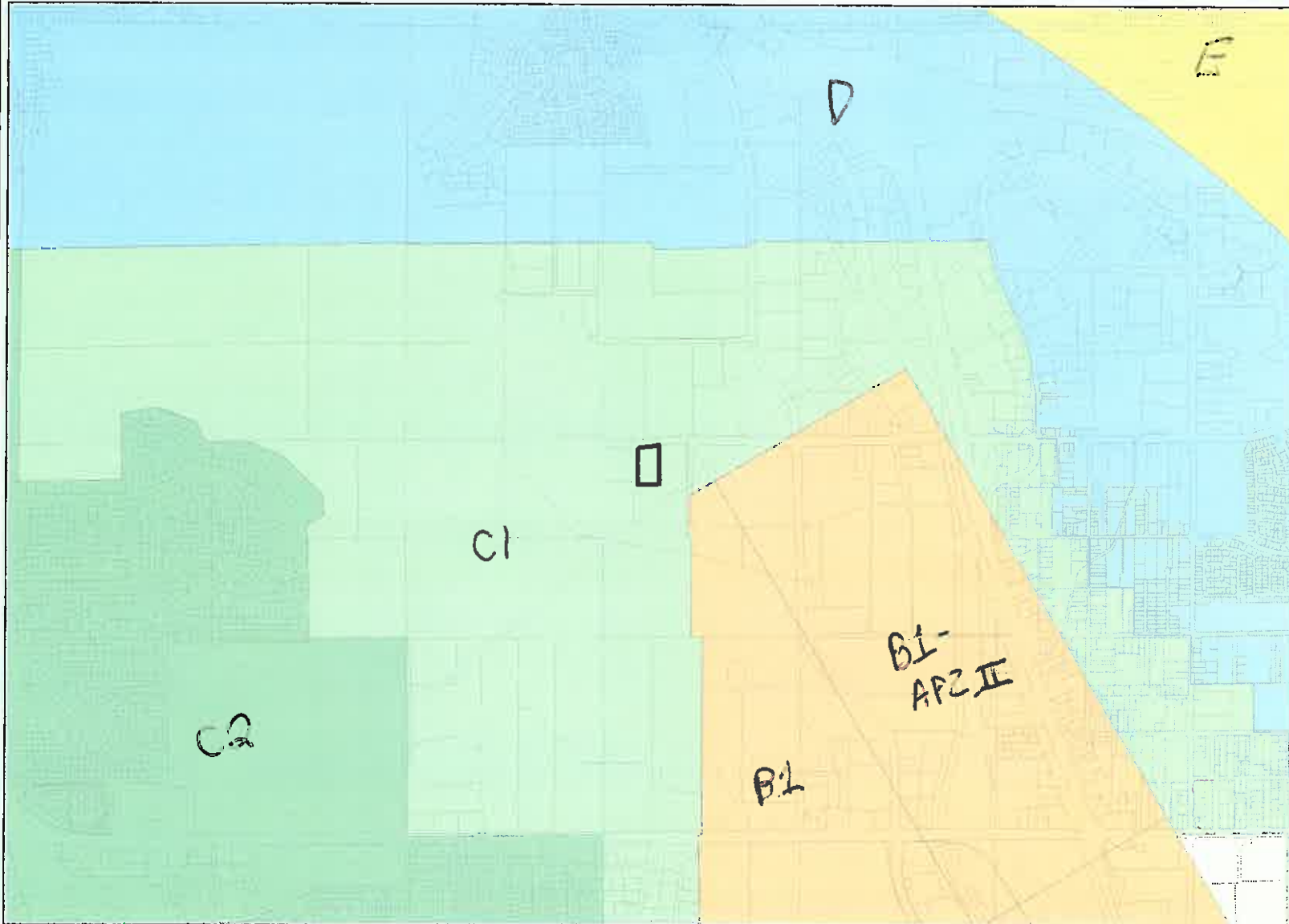
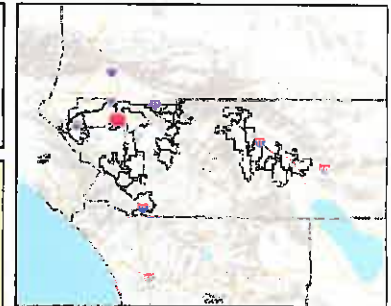
SEE INSET AT RIGHT

Map MA-1
Compatibility Map
March Air Reserve Base / Inland Port Airport

 Google 1615 Eastridge Ave



My Map



Legend

- Airports
- Airport Compatibility**
- OTHER_ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT
- C2-HIGHT-EXC1



0 2,516 5,031 Feet



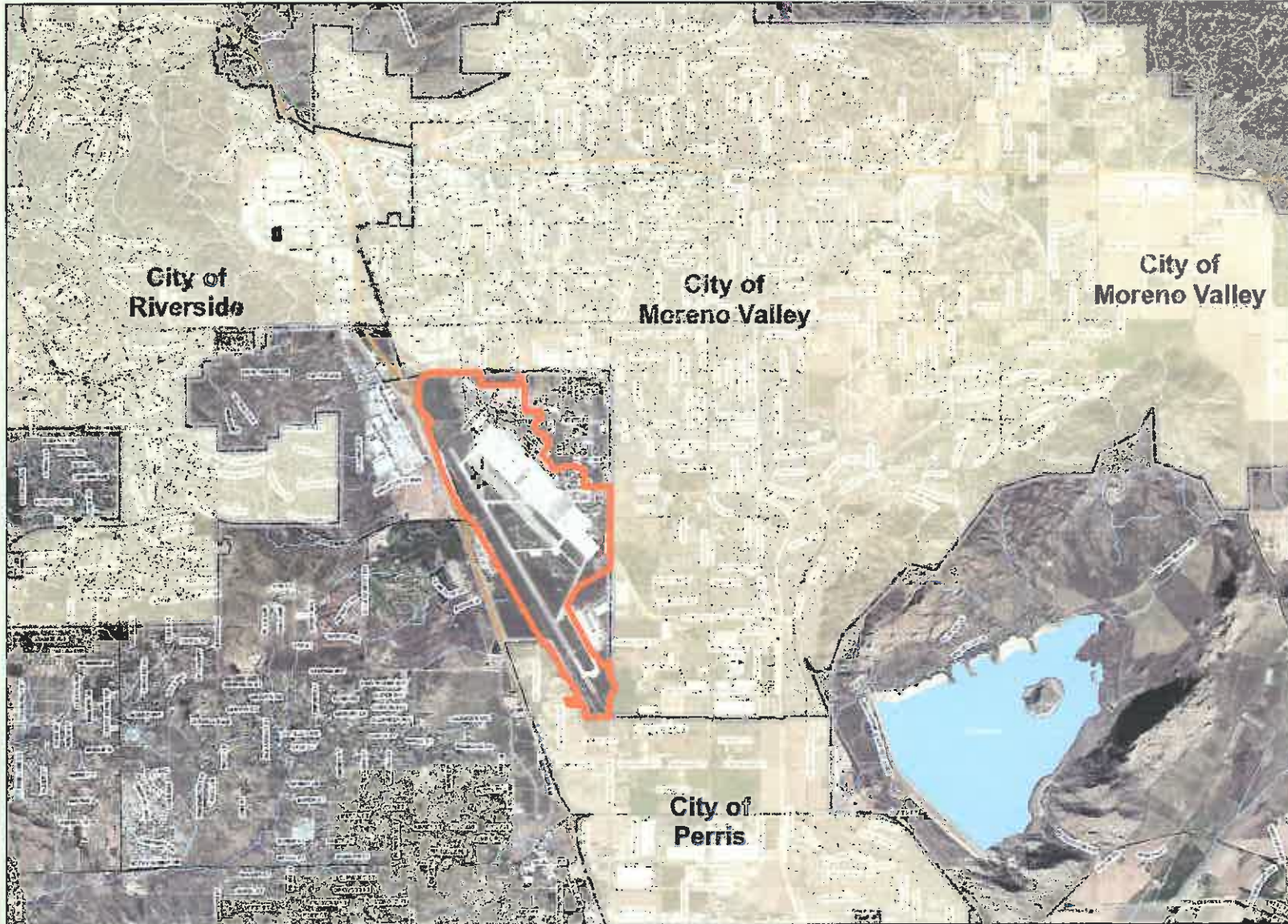
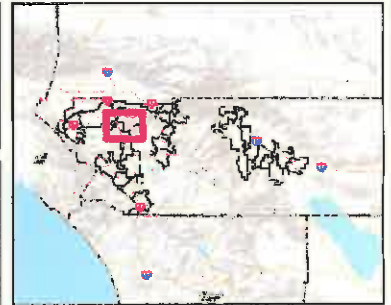
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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Notes

My Map



Legend

- Airports
- City Boundaries
- Cities**
- adjacent_highways**
 - Interstate
 - Interstate 3
 - State Highways; 60
 - State Highways 3
 - US HWY
 - OUT
- highways_large**
 - HWY
 - INTERCHANGE
 - INTERSTATE
 - USHWY
- counties
- cities



0 10,063 20,125 Feet



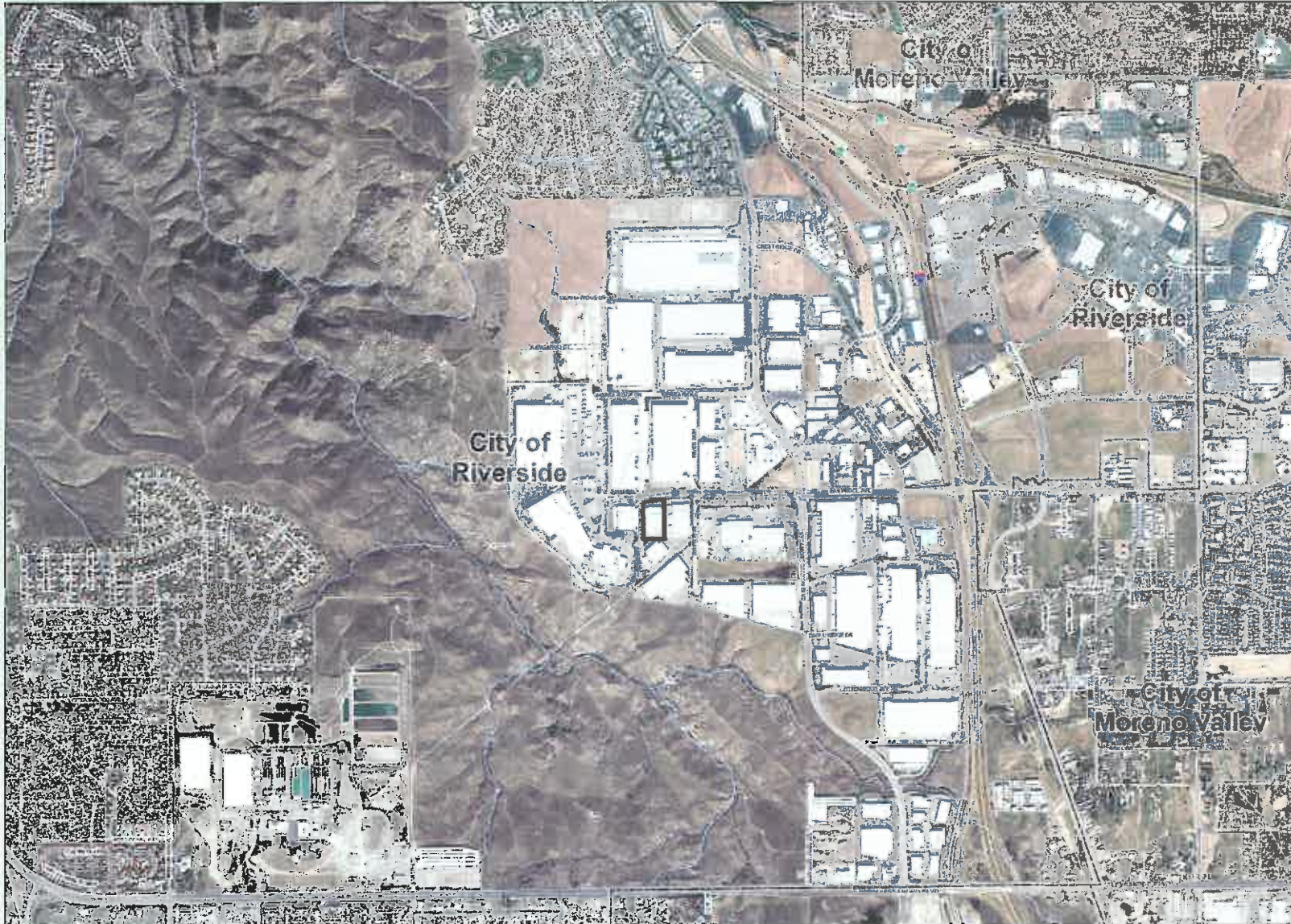
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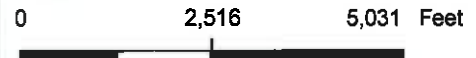
Notes

My Map



Legend

- Airports
- City Boundaries
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



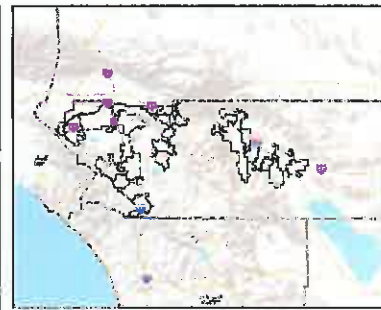
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Notes

My Map



Legend

- RCLIS Parcels
- Airports
- City Boundaries
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 314 629 Feet

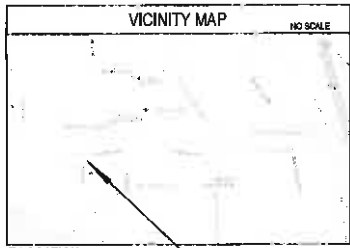


IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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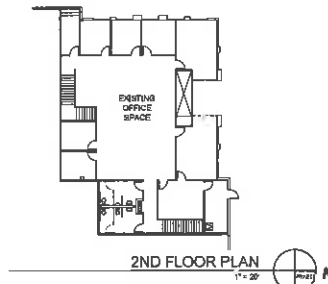
© Riverside County TLMA GIS

Notes



PARKING SUMMARY				
BUILDING	USE	AREA	PARKING REQUIRED	PARKING PROVIDED
1ST FLOOR EXISTING	OFFICE	1,718 S.F.	1060	9
1ST FLOOR NEW	OFFICE	2,097 S.F.	1290	5
1ST FLOOR EXISTING	WAREHOUSE	43,243 S.F.	11000	64
2ND FLOOR EXISTING	OFFICE	2,159 S.F.	1250	9
TOTAL		89,217 S.F.	35740	87
TOTAL REQUIRED PARKING SPACES			90	
TOTAL PROVIDED PARKING SPACES			90	
TOTAL REQUIRED ACCESSIBLE PARKING SPACES			4	
TOTAL PROVIDED ACCESSIBLE PARKING SPACES			4	

NOTE: NECESSARY USED AREA OF 4,444 SQ. FT. (RESTROOMS, L.T. RM., JANITORY CLOSET, HALLS, ENTRANCE, TRAVEL PATHS, STAIRS, BREAK ROOMS, AND WORK ROOMS) ARE NOT INCLUDED IN PARKING ANALYSIS CALCULATIONS.



JENSON U.S.A.

TENANT IMPROVEMENTS

1615 EASTRIDGE AVE.
RIVERSIDE, CALIFORNIA 92507

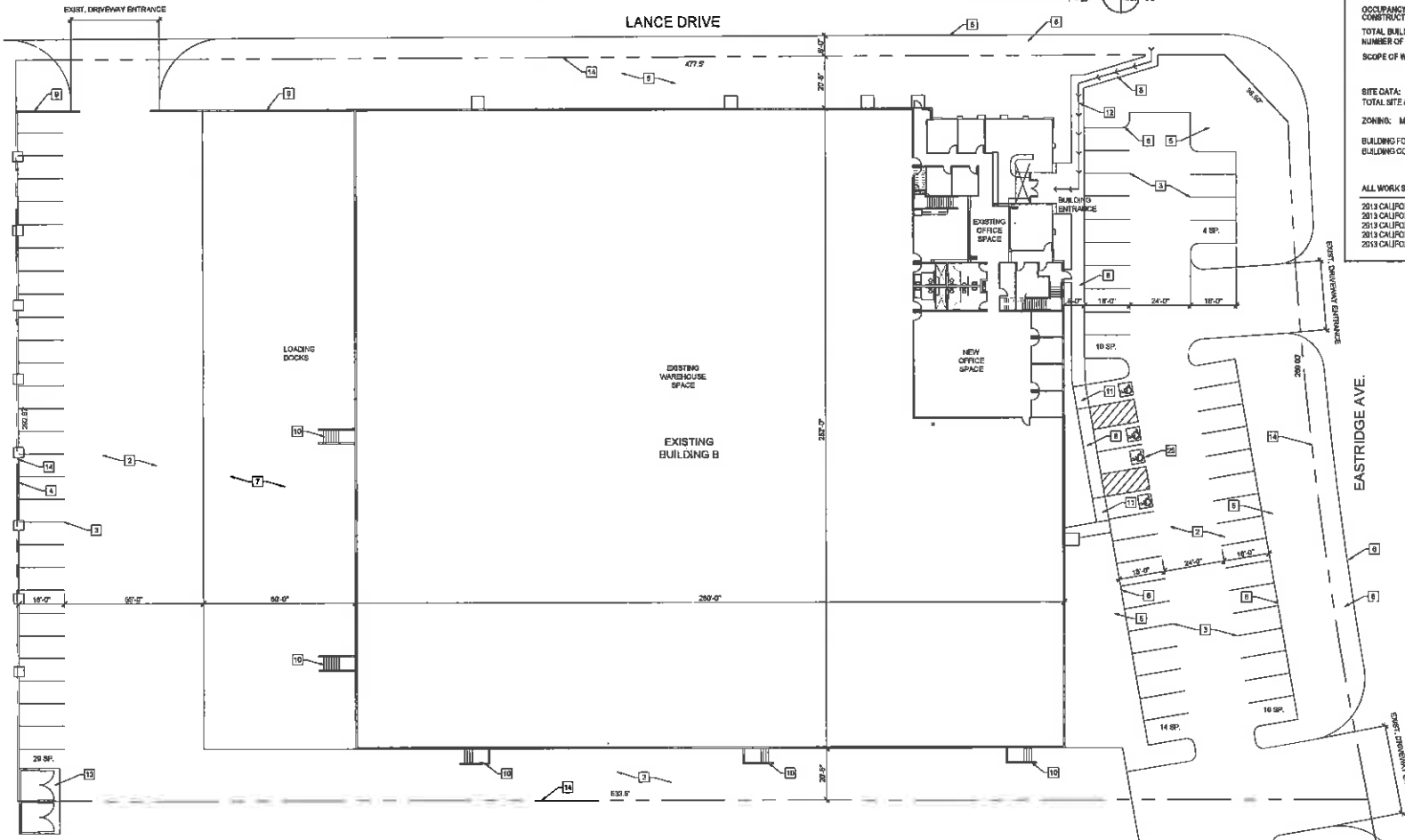
PROJECT DATA

SCOPE OF WORK: ADDITION OF NEW OFFICES INSIDE EXISTING WAREHOUSE FACILITY

OCCUPANCY TYPE: B-3
CONSTRUCTION TYPE: W-3 (SPRINKLED)
TOTAL BUILDING AREA: ~73,712 SQ. FT.
NUMBER OF STORIES: 2
SCOPE OF WORK CONSTRUCTION LIMIT AREA: ~2,610 SQ. FT.

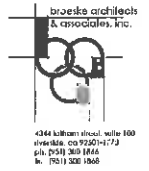
SITE DATA: APN: 953-320-024
TOTAL SITE AREA: 146,722 S.F. (3.44 AC)
ZONING: MP - MANUFACTURING PARK ZONE
BUILDING FOOTPRINT: 70,100 S.F.
BUILDINGS COVERAGE: 41.87%

ALL WORK SHALL COMPLY WITH:
2013 CALIFORNIA BUILDING CODE
2013 CALIFORNIA MECHANICAL CODE
2013 CALIFORNIA PLUMBING CODE
2013 CALIFORNIA ELECTRICAL CODE
2013 CALIFORNIA ENERGY CODE



- SITE PLAN KEYNOTES:**
- 1 EXIST. ACCESSIBLE PARKING STALLS
 - 2 EXIST. ASPHALT PAVING
 - 3 EXIST. PARKING STALLS
 - 4 EXIST. DECORATIVE METAL FENCE
 - 5 EXIST. LANDSCAPE AREA
 - 6 EXIST. CONCRETE CURB
 - 7 EXIST. CONCRETE PAVING
 - 8 EXIST. CONCRETE SIDEWALK
 - 9 EXIST. CONCRETE WALL
 - 10 EXIST. EXTERIOR STAIR
 - 11 EXIST. ACCESSIBLE RAMP
 - 12 ACCESSIBLE PATH OF TRAVEL - MAX. 1:20 SLOPE IN DIRECTION OF TRAVEL, MAX. 1:48 CROSS SLOPE
 - 13 EXIST. TRASH ENCLOSURE
 - 14 EXIST. PROPERTY LINE

SITE PLAN / FLOOR PLAN
T-20



JENSON U.S.A.

TENANT IMPROVEMENTS

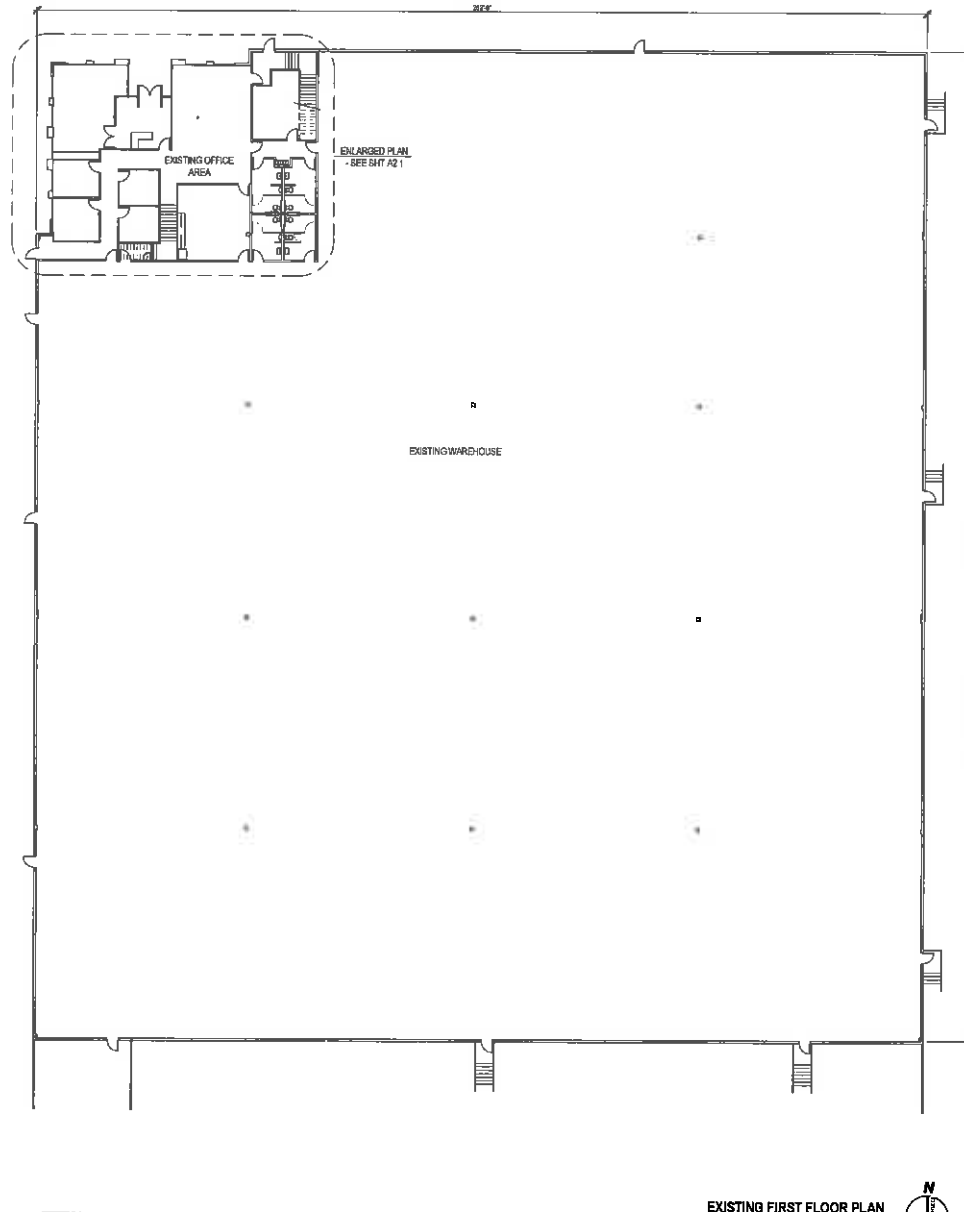
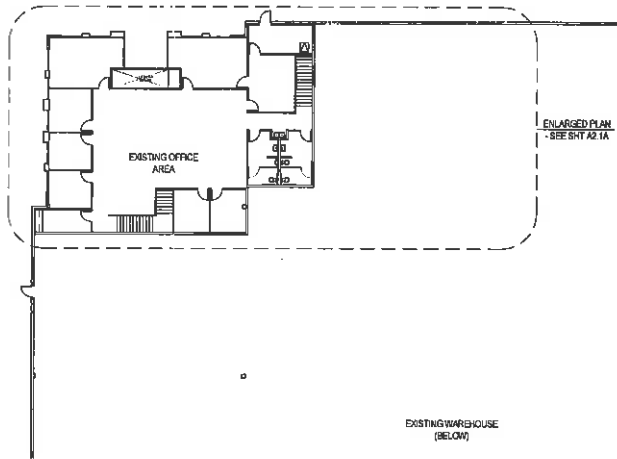
1615 EASTRIDGE AVE. RIVERSIDE CALIFORNIA 92507

NOT TO SCALE - THIS PLAN IS FOR INFORMATION ONLY AND IS NOT TO BE USED FOR CONSTRUCTION. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS ON THE JOB SITE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AGENCIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL AGENCIES.

DATE	
SCALE	AS SHOWN
DRAWN BY	MM/MS
CHECKED BY	MM/MS
DATE	11-20-2013
BY	MM/MS
DATE	

SITE PLAN

A1.1



EXISTING SECOND FLOOR PLAN
1/8" = 1'-0"

BUILDING OCCUPANT LOAD			
OCCUPANCY	AREA IN SQ. FT.	LOAD FACTOR (PER CODE)	# OF OCCUPANTS
GROUP B - WAREHOUSE	60,120	17,000	120
GROUP B - OFFICE, 1ST FLOOR	7,728	17,100	78
GROUP B - OFFICE, 2ND FLOOR	3,657	17,100	31
FUTURE - OFFICE, 2ND FLOOR	12,780	17,100	78
TOTAL	73,685		207

OCCUPANCY LOAD



BUILDING EXTERIOR PHOTOS
BUILDING HEIGHT: 20'-7"

EXISTING FIRST FLOOR PLAN
1/8" = 1'-0"

broetje architects & associates, inc.



4344 Kilborn Street, Suite 100
Berkeley, CA 94704-1773
ph. (925) 307-1844
fx. (925) 300-1828



LEGEND

JENSON U.S.A.
TENANT IMPROVEMENTS
1818 EASTSIDE AVE., INVERSIDE CALIFORNIA 90607

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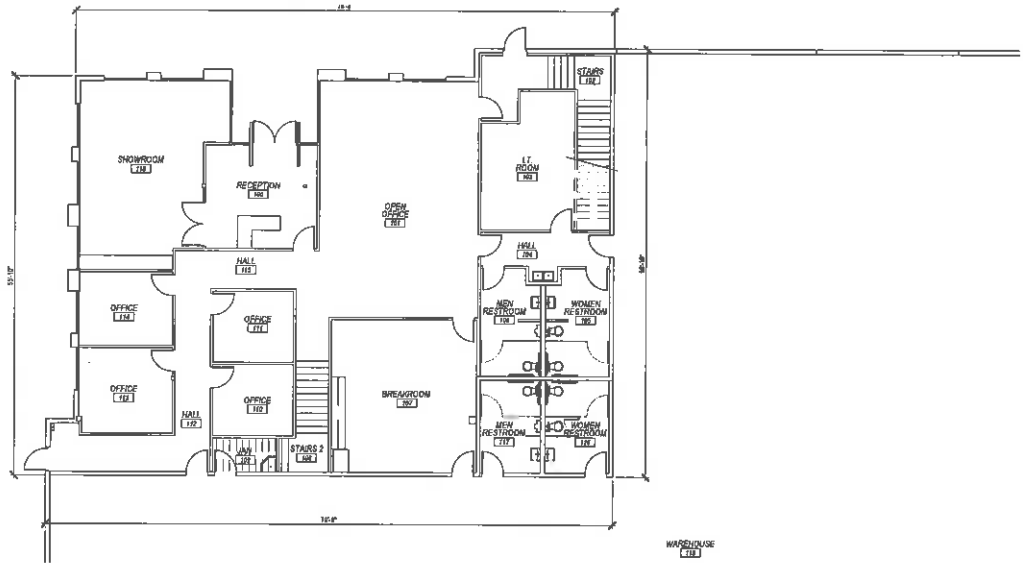
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SCALE	AS SHOWN
DESIGNER	BA
CHECKER	BA
DATE	
PROJECT	

04/11/15

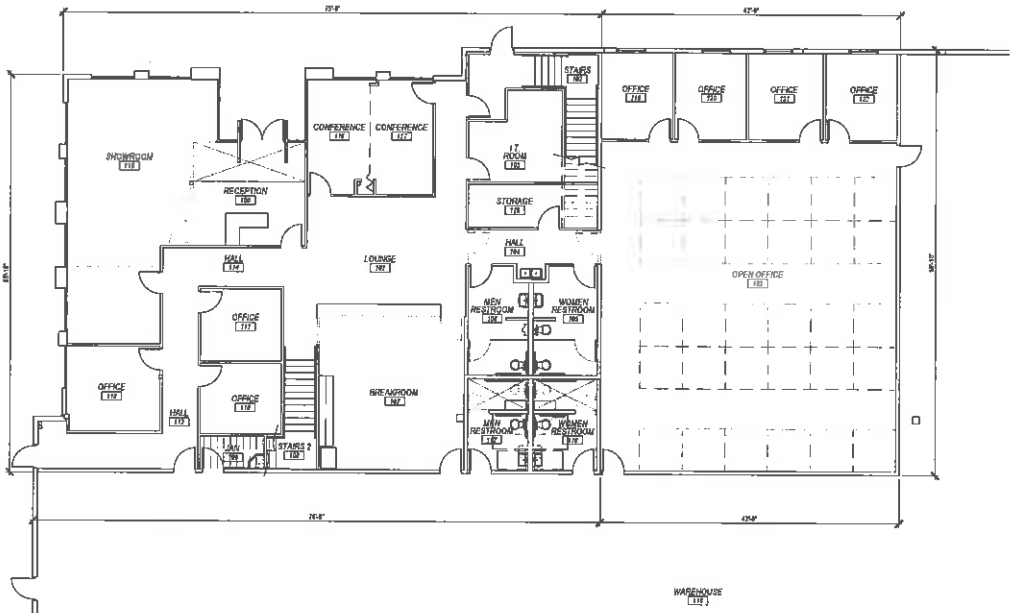
EXISTING FLOOR PLANS

04/11/15

A2.0



ENLARGED EXISTING FIRST FLOOR PLAN



ENLARGED PROPOSED FIRST FLOOR PLAN



broeke architects
& associates, inc.



4744 Edison Street, Suite 100
Irving, CA 92604-1772
(916) 291-1666
D. (510) 290-1625



01/26/2011

JENSON U.S.A.
TENANT IMPROVEMENTS
1015 EASTSIDE AVE. IRVING, CALIFORNIA 92607

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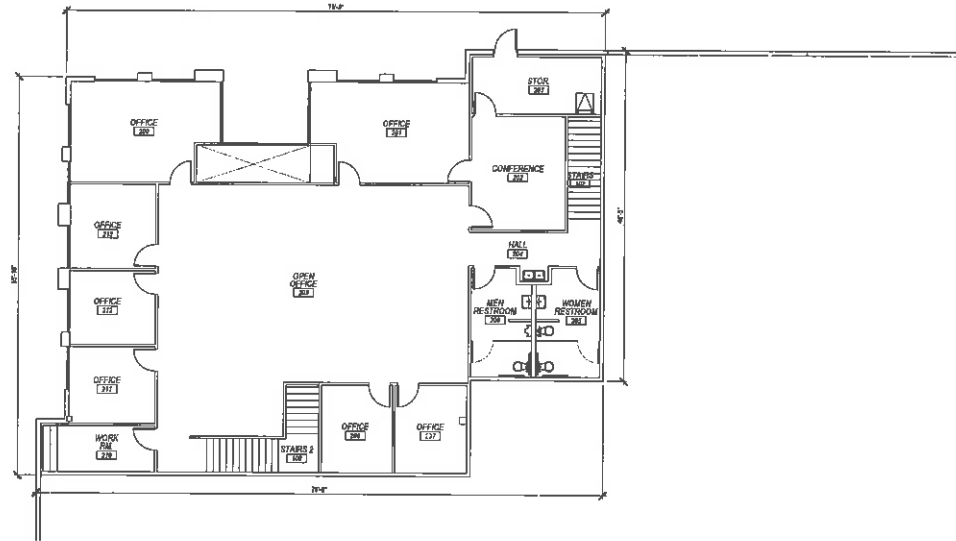
DATE:	1/26/11
DESIGNER:	LB
CHECKED:	LB
DATE:	1/26/11
PROJECT:	JENSON
LOCATION:	1015 EASTSIDE AVE. IRVING, CA
SCALE:	AS SHOWN
REVISIONS:	

01/26/11

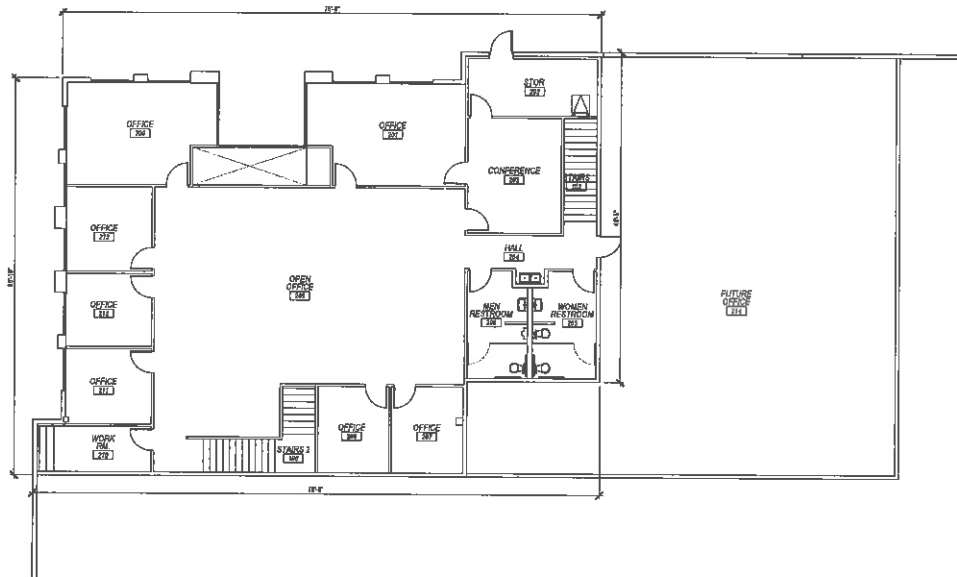
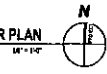
PROPOSED FLOOR PLAN

01/26/11

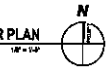
A2.1



ENLARGED EXISTING SECOND FLOOR PLAN



ENLARGED PROPOSED SECOND FLOOR PLAN



brooke architec & associates, inc.



4344 Wilshire Blvd. Suite 10
Beverly Hills, CA 90210-1173
Tel: (310) 300-1866
Fax: (310) 300-1868



Sheet No.:

JENSON U.S.A.
TENANT IMPROVEMENTS
101 E. BAS BRIDGE AVE., INVERGLENDE, CALIFORNIA 90029

Not shown, unless otherwise indicated, are existing conditions. All work shall be in accordance with the applicable building codes and regulations. The architect shall not be responsible for the accuracy of the information provided by the owner or other sources. The architect shall not be responsible for the accuracy of the information provided by the owner or other sources. The architect shall not be responsible for the accuracy of the information provided by the owner or other sources.

AW	=	As Shown
MR	=	Minor
MA	=	Major
DA	=	Demolish
PR	=	Proposed
OR	=	Other
ST	=	Structural
ME	=	Mechanical
EL	=	Electrical
PL	=	Plumbing
SI	=	Site
PA	=	Particulars

PROPOSED FLOOR PLAN

A2.1A





PREVIOUS REVIEW OF BUILDING

COUNTY OF RIVERSIDE AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM: VI. G.

HEARING DATE: May 22, 2003

CASE SUMMARY

CASE NUMBER: MA-03-120-Carter Redish
APPROVING JURISDICTION: City of Riverside
JURISDICTION CASE NO: Plot Plan P03-0163

PROJECT DESCRIPTION:

Four industrial buildings totaling 301,000 sq. ft. on approximately 16 acres.

PROJECT LOCATION:

The site is situated south of Eastridge between Lance Drive and River Run within the City of Riverside, approximately 15-16,000 ft. northwest of Runway 14/32 at March Air Reserve Base.

Adjacent Airport: March Air Reserve Base/March Inland Port

- a. Airport Influence Area: Within Area of Influence Study Area
- b. Land Use Policy: Influence Area II
- c. Noise Levels: See Below

BACKGROUND:

The ALUC has been active in protecting the airport from intrusion since the inception of the Commission in the early 1970's. The first AIR INSTALLATION COMPATIBILITY USES ZONE (AICUZ) protection was initiated by a Board of Supervisors request in November of 1971. The original Interim Influence Area was designated in February of 1972 and was redrawn in 1975 based upon a 1972 AICUZ.

In 1983 the ALUC redrew the boundaries to reflect the 1979 AICUZ. In April of 1984 the ALUC adopted the Riverside County Airport Land Use Plan (RCALUP). In May of 1986 the ALUC again redrew the boundaries to reflect the 1983 AICUZ. In 1992 and again in 1998 the AICUZ reports were redone to reflect the mission changes of the two Base Realignments; however, no changes were made to the Interim Influence Zone created in 1986.

In 1990 the ALUC was able to obtain Department of Defense funding for a Comprehensive Land Use Plan (CLUP) that resulted in the 1994 Draft. This was about the time that the second base realignment was announced and it was consequently never adopted. The current 98/99 Draft CLUP efforts was prepared utilizing the 1998 AICUZ in conjunction with the 1993 CalTrans Handbook.

Since we have not adopted the CLUP for MARB, we will utilize five resources for our review:

1. RCALUP: 1984 with Interim boundaries for March Air Force Base: 1986
2. CalTrans Airport Land Use Planning Handbook: 2002
3. Draft CLUP for March Air Force Base: 1994
4. Noise Data from the AICUZ Study: 1998 March Air Reserve Base
5. Draft 98/99 CLUP for MARB/MIP

MAJOR ISSUES:

Land Use: The proposal is for an industrial development on approximately 16.54 acres. The proposed site is located approximately 15,000 ft. north of Runway 14/32. The proposal is near a major flight track and within the conical surface.

The 1984 Plan places an emphasis upon the type of airport, the type of aircraft using the airport, planned and existing approach profiles, actual flight tracks, noise levels, or a combination of these factors. The site is located in Area II, which allows commercial and industrial land use with a few restrictions. The 1994 Draft CLUP placed the property outside of the 60 CNEL. The proposed land use designation would be consistent with allowed land uses within this area contingent upon noise and height issues.

Density and Coverage: The area of the proposed structures is approximately 301,000 sq. ft. The lot area is approximately 720,482 sq. ft. (net). Structural coverage will be 42% of the net area.

Part 77: The highest elevation at the site is 1,527 MSL feet and the height of the tallest structure is approximately 36 ft. Any structures over 1,685 MSL feet in elevation will require an FAA 7460 review.

Noise: The site has been shown to have some noise over the property with each of the AICUZ reports. The 1998 AICUZ indicated the noise level at the property to be 60+CNEL.

CONDITIONS:

1. Prior to project development or sale to an entity exempt from the Subdivision Map Act, the project proponents shall convey an avigation easement to the MARB/MIP Airport. (Tel. 909-656-7000)
2. Incorporate noise attenuation measures into the office portions of the building construction to ensure interior noise levels are at or below 45-decibel levels.

3. Install hooded or shielded outdoor lighting measures into the building construction to ensure that all light is below the horizontal plane.
4. The following uses shall be prohibited:
 - a. Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - b. Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - c. Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area.
 - d. Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
5. The above ground storage of explosive or flammable materials is prohibited.
6. Structures exceeding 1,685 MSL feet in elevation shall be submitted to the FAA for review.
7. Subsequent use for the property shall be reviewed by the ALUC.

RECOMMENDATION: Staff recommends a finding of consistency for the project subject to the conditions outlined above.

NOTICE OF PUBLIC HEARING

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Thursday, from 8:00 a.m. to 5:00 p.m., except Monday, September 7 (Labor Day).

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon St., 1st Floor Hearing Room
Riverside, California

DATE OF HEARING: September 10, 2015

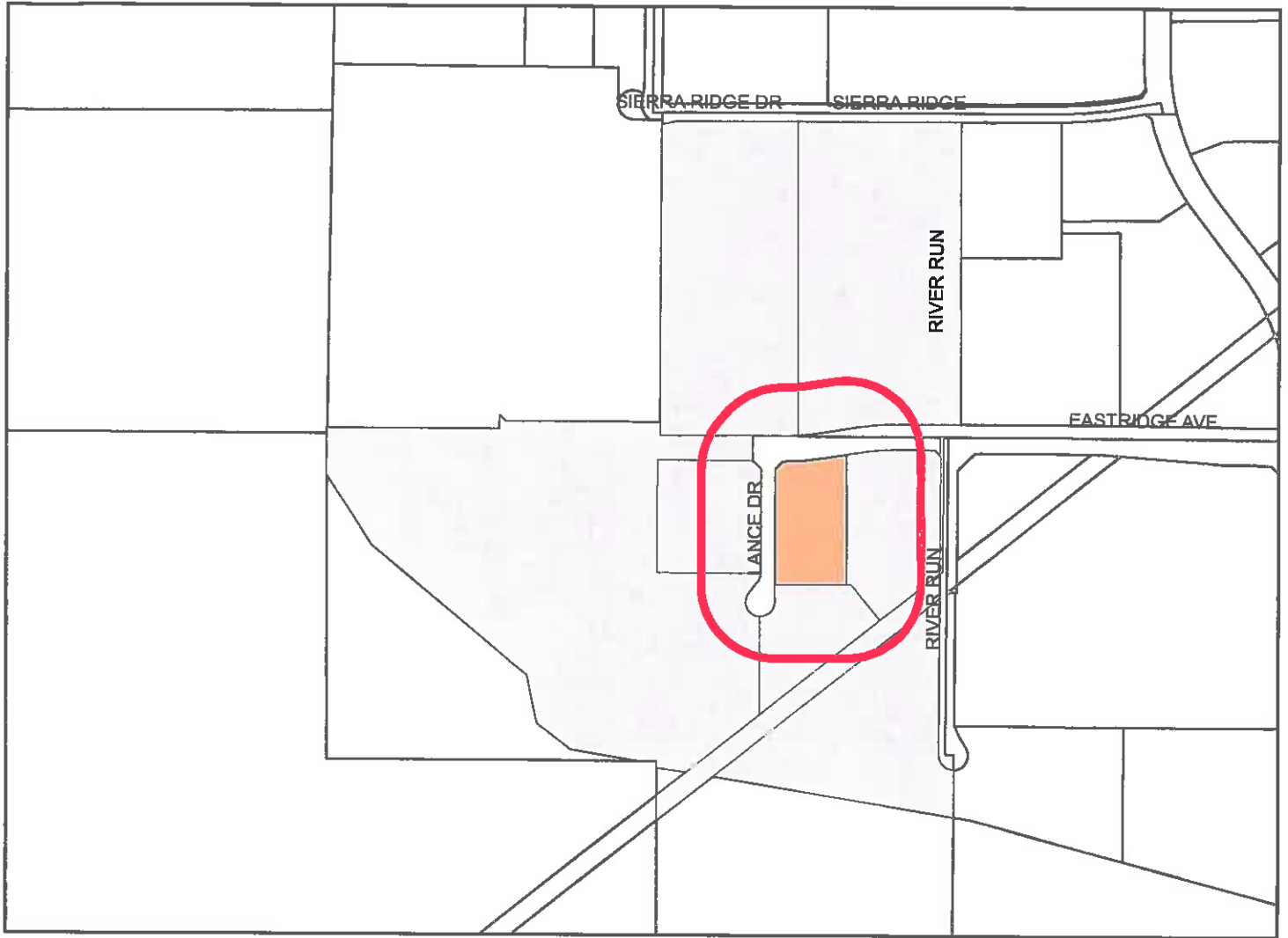
TIME OF HEARING: 9:00 A.M.

CASE DESCRIPTION:

ZAP1138MA15 – R.C. Jenson, Inc., dba Jenson USA (Representative: Jim Broeske, Broeske Architects & Associates, Inc.) - City of Riverside Building Department. The applicant proposes to add 2,500 square feet of first floor office area and 2,730 square feet of second floor office area within an existing industrial warehouse building with an address of 1615 Eastridge Avenue, located at the southeasterly corner of Eastridge Avenue and Lance Drive. These improvements would increase the net building area to 73,652 square feet. (Airport Compatibility Zone C1 of the March Air Reserve Base/Inland Port Airport Influence Area).

FURTHER INFORMATION: Contact Russell Brady at (951) 955-0549 or John Guerin at (951) 955-0982. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions.

Apn: 263-320-024 (300 feet buffer)



Selected Parcels

263-320-026 263-320-016 263-320-025 263-320-024 263-320-029 263-320-013 263-290-070 263-290-071 263-320-027



525 262.5 0 525 Feet

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APPLICATION FOR MAJOR LAND USE ACTION REVIEW
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

ALUC Identification No.

ZAP1138MA15

PROJECT PROPONENT (TO BE COMPLETED BY APPLICANT)

Date of Application July 27, 2015
 Property Owner RC Jensen, Inc. Phone Number (951) 344-1010
 Mailing Address 1615 Eastridge Ave. Riverside, CA 92507

Agent (if any) Broeske Architects & Assoc. Phone Number (951) 300-1866 # 2245
 Mailing Address 4344 Latham St. #100
Riverside CA. 92501
Attn: James Broeske

PROJECT LOCATION (TO BE COMPLETED BY APPLICANT)

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address 1615 Eastridge Ave.
Riverside CA 92507
 Assessor's Parcel No. APN - 263-320-024 Parcel Size 3.94 Ac.
 Subdivision Name _____ Zoning _____
 Lot Number _____ Classification MP - Manufacturing Park

PROJECT DESCRIPTION (TO BE COMPLETED BY APPLICANT)

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe) The facility is a fill-up concrete office warehouse building. It is two story and similar to all adjacent buildings in the Park

Proposed Land Use (describe) There is no change in land use proposed.
This is a simple 5,000 s.f. tenant improvement to the interior of the existing warehouse. No new exterior height increases

For Residential Uses Number of Parcels or Units on Site (exclude secondary units) NA
 For Other Land Uses Hours of Use 7:00 AM to 6:00 PM
 (See Appendix C) Number of People on Site Maximum Number 171 Calculated; (96 Actual)
 Method of Calculation Office area @ 1/200 s.f. (See Attachment)
Warehouse @ 1/500 s.f.

Height Data Height above Ground or Tallest Object (including antennas and trees) No Exterior Changes Proposed
 Highest Elevation (above sea level) of Any Object or Terrain on Site Tenant Improvement ft.

Flight Hazards Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight? Yes
 No
 If yes, describe _____

REFERRING AGENCY (APPLICANT OR JURISDICTION TO COMPLETE)	
Date Received	
Agency Name	Riverside Buildings Dept.
Staff Contact	Kyle Smith
Phone Number	(951) 826-5220
Agency's Project No.	NA
Type of Project	<input type="checkbox"/> General Plan Amendment <input type="checkbox"/> Zoning Amendment or Variance <input type="checkbox"/> Subdivision Approval <input type="checkbox"/> Use Permit <input type="checkbox"/> Public Facility <input checked="" type="checkbox"/> Other <i>Tenant Improvement</i>

A. **NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. **SUBMISSION PACKAGE:**

ALUC REVIEW

- 1 Completed Application Form
- 1 Project Site Plan – Folded (8-1/2 x 14 max.)
- 1 Elevations of Buildings - Folded
- 1 Each . 8 ½ x 11 reduced copy of the above
- 1 8 ½ x 11 reduced copy showing project in relationship to airport.
- 1 Set . Floor plans for non-residential projects
- 4 Sets . Gummed address labels of the Owner and representative (*See Proponent*).
- 1 Set . . Gummed address labels of all property owners within a 300' radius of the project site. If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10), with ALUC return address.
- 4 Sets . Gummed address labels of the referring agency (City or County).
- 1 Check for Fee (See Item "C" below)

STAFF REVIEW (Consult with ALUC staff planner as to whether project qualifies)

- 1 Completed Application Form
- 1 Project Site Plans – Folded (8-1/2 x 14 max.)
- 1 Elevations of Buildings - Folded
- 1 8 ½ x 11 Vicinity Map
- 1 Set . Gummed address labels of the Owner and representative (*See Proponent*).
- 1 Set . Gummed address labels of the referring agency.
- 1 Check for review—See Below

OCCUPANCY CALCULATIONS

BUILDING	USE	AREA	DENSITY	OCCUPANCY
1ST FLOOR EXISTING	OFFICE	1,718 S.F.	1/200	9
1ST FLOOR NEW	OFFICE	2,097 S.F.	1/200	11
1ST FLOOR EXISTING	WAREHOUSE	63,283 S.F.	1/500	127
2ND FLOOR EXISTING	OFFICE	2,168 S.F.	1/200	11
2ND FLOOR NEW	OFFICE	2,500 S.F.	1/200	13
TOTAL		71,766 S.F.		171

TOTAL SITE AREA (3.44 ACRES) 149,722 SQ. FT.

NOTE:

ACCESSORY USED AREAS OF 4,446 SQ. FT. (RESTROOMS, I.T. RM., JANITOR'S CLOSET, HALLS, ENTRANCE, TRAVEL PATHS, STAIRS, BREAK ROOMS, AND WORK ROOMS) ARE NOT INCLUDED IN OVERALL TOTAL OCCUPANCY CALCULATIONS, ON THAT SPACES ARE USED BY EMPLOYEES OF THE FACILITY.

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.2

HEARING DATE: September 10, 2015

CASE NUMBER: ZAP1135MA15 – Jack Kofdarali and Ino Cruz/French Valley
Benton Rd. LP

APPROVING JURISDICTION: City of Moreno Valley

JURISDICTION CASE NO: PA15-0032 (Conditional Use Permit)

MAJOR ISSUES: The square footage of dining and serving areas within the two freestanding restaurants will have to be limited to 1,500 square feet each in order to ensure compliance with Airport Compatibility Zone B2 single-acre intensity limits. The project has been designed to comply with these limits, and conditioned accordingly. The warehouse/industrial building requires FAA Obstruction Evaluation review at the proposed height and top point elevation.

RECOMMENDATION: Staff recommends that the Commission CONTINUE this matter to October 8, 2015, pending FAA Obstruction Evaluation submittal as requested by staff. However, if submittal is made to FAA as requested by staff and is acknowledged as a “Work in Progress” prior to the hearing, staff would recommend a finding of CONDITIONAL CONSISTENCY for the Conditional Use Permit, subject to the conditions included herein.

PROJECT DESCRIPTION: The applicant proposes to develop and operate a 48,833 square foot industrial warehouse (including 5,446 square feet of office space), a gasoline and diesel service station with 14 pumps, a 3,800 square foot convenience store, three fast-food restaurants with a combined gross floor area of 9,200 square feet, and a car wash on 6.31 acres.

PROJECT LOCATION: The site is located northerly of Cactus Avenue, easterly of Commerce Center Drive, and southerly of Goldencrest Drive, within the City of Moreno Valley, approximately 5,000 feet northerly of the northerly end of Runway 14-32 at March Air Reserve Base.

LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

- a. Airport Influence Area: March Air Reserve Base
- b. Land Use Policy: Airport Compatibility Zone B2
- c. Noise Levels: Approximately 65 CNEL

BACKGROUND:

Non-Residential Average Land Use Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport (March ALUCP), the site is located within Compatibility Zone B2. Zone B2 limits average intensity to 100 people per acre.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, the intensity of restaurant serving areas is evaluated as one person per 15 square feet, commercial kitchen area as one person per 200 square feet, storage areas as one person per 300 square feet, office areas as one person per 100 square feet, and warehouse areas as one person per 500 square feet. However, Appendix C recommends that, for calculation of intensity levels, the Building Code occupancy levels be reduced by 50 percent, at least for office uses. Pursuant to special policies for the March ALUCP, the intensity of retail areas is evaluated as one person per 115 square feet.

The applicant's title sheet indicates that the warehouse would have a total occupancy of 168 persons, and that the three fast food restaurants would have a total occupancy of 263 persons. Using the special retail area intensity of one person per 115 square feet, the convenience store would accommodate 33 persons. Adding 20 vehicles at the gas pumps, 30 vehicles in the three drive-thrus, and 8 trucks at the diesel pumps, and assuming 1.5 persons per automobile and 1.0 person per truck, the mobile occupancy of the site would be 83 persons, resulting in a total on-site occupancy of 547. This number results in an average occupancy for a 6.31-acre site of 87 persons per acre, which is consistent with the Airport Compatibility Zone B2 criterion of 100 persons per acre.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per standard vehicle and 1.0 persons per trailer truck in the absence of more precise data). Based on the number of parking spaces provided for the site (191 standard vehicle spaces plus the 58 mobile sites cited above, including fuel pumps and drive-thru queues), the total site occupancy would be estimated at 370 persons. This number results in an average occupancy for a 6.31-acre site of 59 persons per acre, which is consistent with the Airport Compatibility Zone B2 criterion of 100 persons per acre.

Non-Residential Single-Acre Land Use Intensity: As noted above, the site is located within Compatibility Zone B2. Zone B2 limits maximum single-acre intensity to 250 people. There are no risk-reduction design bonuses available, as March Air Reserve Base/Inland Port Airport is primarily utilized by large aircraft weighing more than 12,500 pounds.

The most intense single-acre area on the site includes the two freestanding fast food restaurants, the drive-thru for one of these restaurants, and the southerly 30 feet of the industrial warehouse building (approximately 5,790 square feet). Provided that the southerly 30 feet of the industrial warehouse building is utilized specifically for warehouse uses at one person per 500 square feet, that area would accommodate 12 persons. The drive-thru queue (assuming 10 vehicles) would accommodate 15 persons.

The applicant's plans indicate a maximum of 1,300 square feet of dining/serving area in each of the two fast food restaurants. If we assume that the remaining 1,900 square feet is kitchen and office area, each restaurant would accommodate 97 persons, for a subtotal of 194, and a single-acre total of 221, which is consistent with the Airport Compatibility Zone B2 criterion of 250 persons maximum in any given acre.

Given the above information, we can allow for the restaurants to have a serving area up to 1,500 square feet apiece, with the remaining areas of 1,700 square feet. In that situation, each restaurant could accommodate 110 persons for a subtotal of 220, and a single-acre total of 247, which would still be consistent with the single-acre criterion of 250 persons.

Prohibited and Discouraged Uses: The project does not propose any other uses prohibited or discouraged in Compatibility Zone B2. The use of buildings may change over time. Therefore, staff has included recommended conditions prohibiting uses that are not permissible in this Compatibility Zone.

Projects within Compatibility Zone B2 are required to locate structures a maximum distance from the runway. In this case, the site is well removed from both the runway and the extended runway centerline. Additionally, it should be noted that the traffic pattern for March Air Reserve Base/Inland Port Airport is located west of the extended runway centerline.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being located at approximately 65 CNEL with the noise contour crossing the project site. While the proposed warehouse is not a noise-sensitive use and would not require special measures to mitigate aircraft-generated noise, such measures may be required to achieve reduced interior noise levels of 45 dBA CNEL in office areas as required pursuant to the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan.

Part 77: The elevation of Runway 14-32 at its northerly terminus is approximately 1535 feet above mean sea level (1535 feet AMSL). At a distance of approximately 5,000 feet from the runway, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1585 feet AMSL. The proposed buildings have a finished floor elevation of approximately 1554 feet AMSL. Most of the buildings are less than 25 feet in height. The warehouse, however, has a maximum height of 36 feet, resulting in a top point elevation of 1590 feet AMSL. Therefore, the applicant has been advised to file Form 7460-1 online with the Federal Aviation Administration Obstruction Evaluation Service for the proposed warehouse building.

Open Area: None of the Compatibility Zones for the March ALUCP require open area specifically.

CONDITIONS:

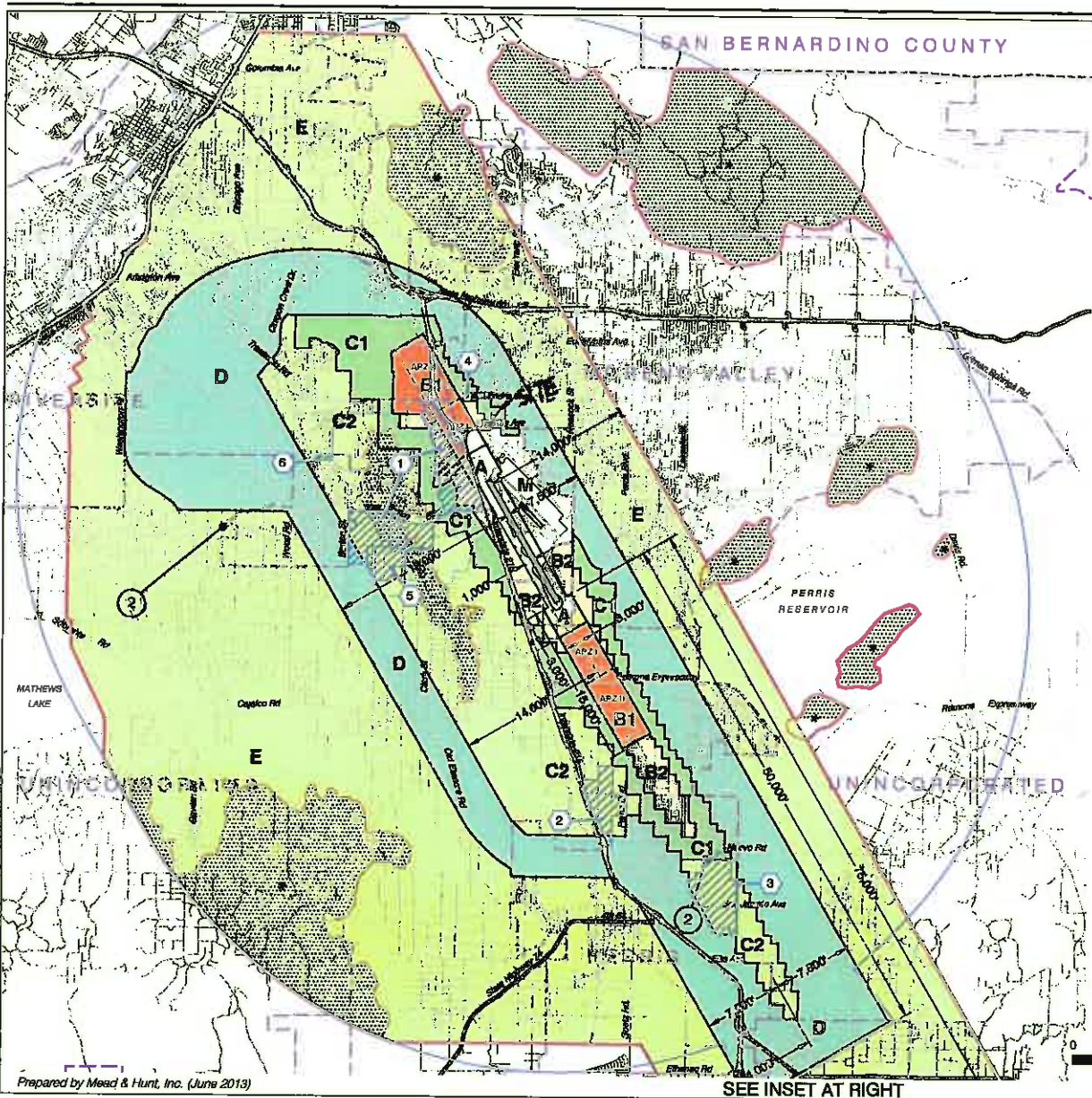
1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.

2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, congregate care facilities, hotels/motels, places of assembly, noise sensitive outdoor nonresidential uses and hazards to flight.
3. Prior to issuance of any building permits, the landowner shall convey and have recorded an aviation easement to the March Inland Port Airport Authority. Contact March Joint Powers Authority at (951) 656-7000 for additional information.
4. The attached notice shall be given to all prospective purchasers and/or tenants of the property. While not required, the applicant and its successors-in-interest are encouraged to provide a copy of said notice to employees who would regularly be working at this location.
5. Any new detention basin(s) on the site (including bioswales) shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the detention basin(s) shall not include trees that produce seeds, fruits, or berries.

6. The City of Moreno Valley shall require that an acoustical analysis be performed prior to issuance of building permits for the warehouse/office building to determine whether sound attenuation features are sufficient to reduce interior noise from aircraft to no more than 45 dBA CNEL in office areas.
7. Overall office and manufacturing area within the warehouse/office building shall be limited to a maximum of 5,446 square feet in the absence of further review by ALUC. The southerly 30 feet of the building shall be limited to warehouse use only. If any development of the industrial building proposes to exceed the maximum office and manufacturing area, or if any use other than warehousing is proposed in the remaining area, further ALUC review shall be required to determine its consistency with the applicable criteria in place at that time.
8. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
9. The dining/serving area in each of the freestanding fast food restaurants shall be limited to 1,500 square feet apiece. (The remaining area may be utilized as commercial kitchen, storage, etc., including one manager's office.)

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



LEGEND

Compatibility Zones

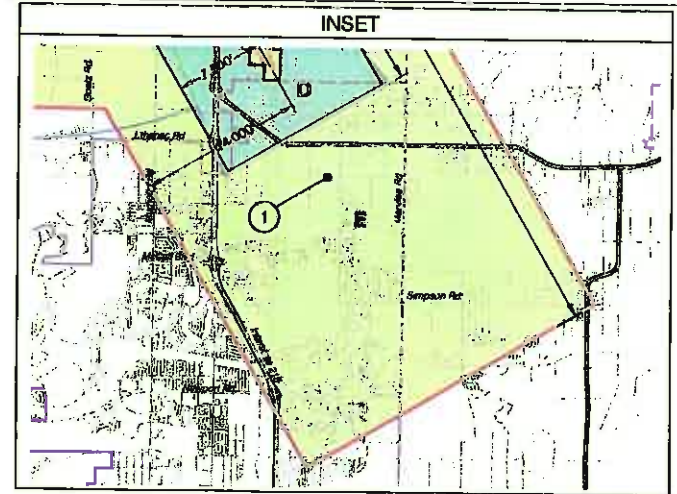
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

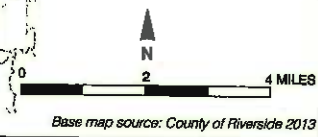
- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



**Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)**

Note:
All dimensions are measured from runway ends and centerlines.



Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map MA-1

Compatibility Map
March Air Reserve Base / Inland Port Airport



Alessandro Blvd

Old 15 Frontage Rd

Elsworth St

Frederick St

Graham St

Hancock St

Cactus Ave

Riverside Dr

Hancock St

WEST MARCH

Meyer Dr

ARNOLD HEIGHTS

March Air Reserve Base

March Air Reserve Base

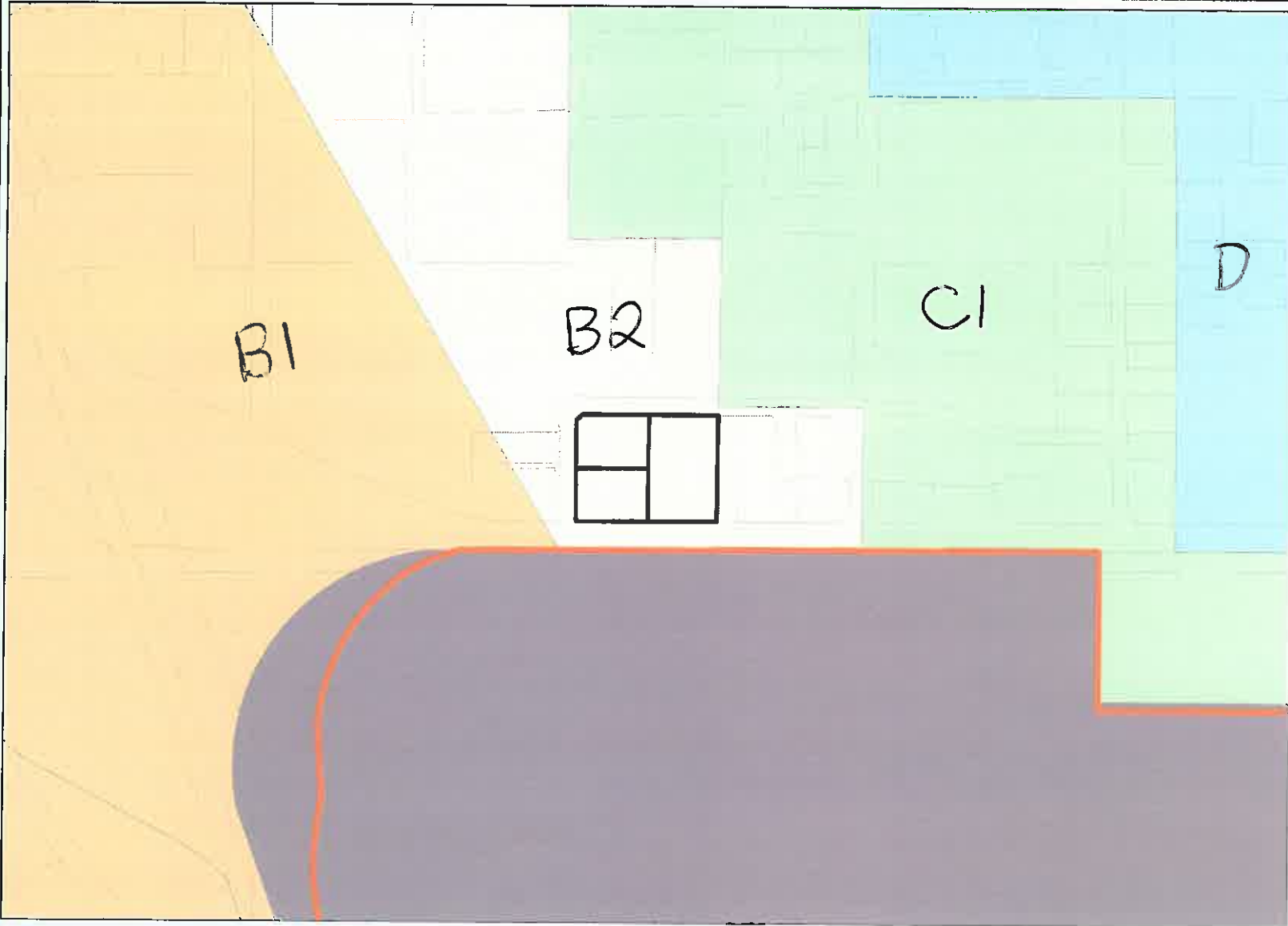
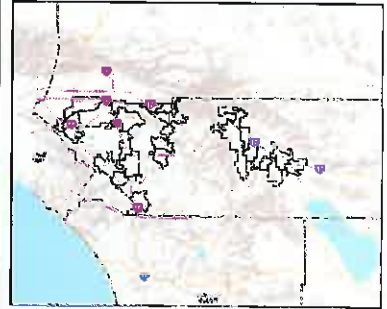
ALESSANDRO

Riverside National Cemetery

General Golf

Google

My Map



Legend

- RCLIS Parcels
- Airports
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



0 799 1,598 Feet



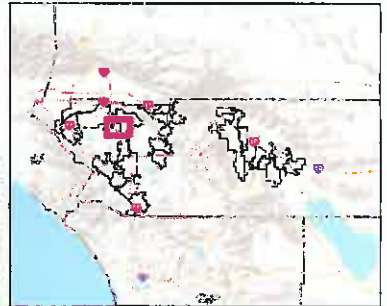
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Notes

My Map



- Legend**
- Airports
 - Intake Boundaries**
 - <all other values>
 - NO
 - UNKNOWN
 - YES
 - Historic Preservation Districts
 - City Boundaries
 - highways_large**
 - HWY
 - INTERCHANGE
 - INTERSTATE
 - USHWY
 - majorroads
 - counties
 - cities



0 6,390 12,781 Feet



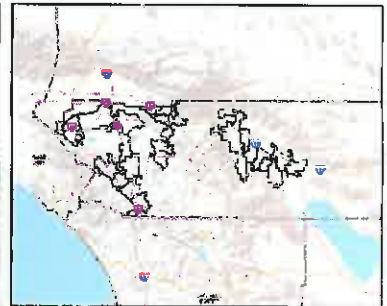
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Notes

My Map



Legend

- RCLIS Parcels
- Airports
- Intake Boundaries
- <all other values>
- NO
- UNKNOWN
- YES
- Historic Preservation Districts I
- City Boundaries
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers

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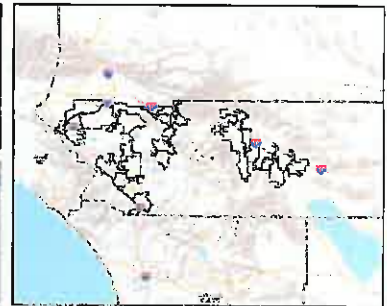


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Notes

My Map



Legend

- RCLIS Parcels
- Airports
- Intake Boundaries
- <all other values>
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- UNKNOWN
- YES
- Historic Preservation Districts I
- City Boundaries
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 399 799 Feet

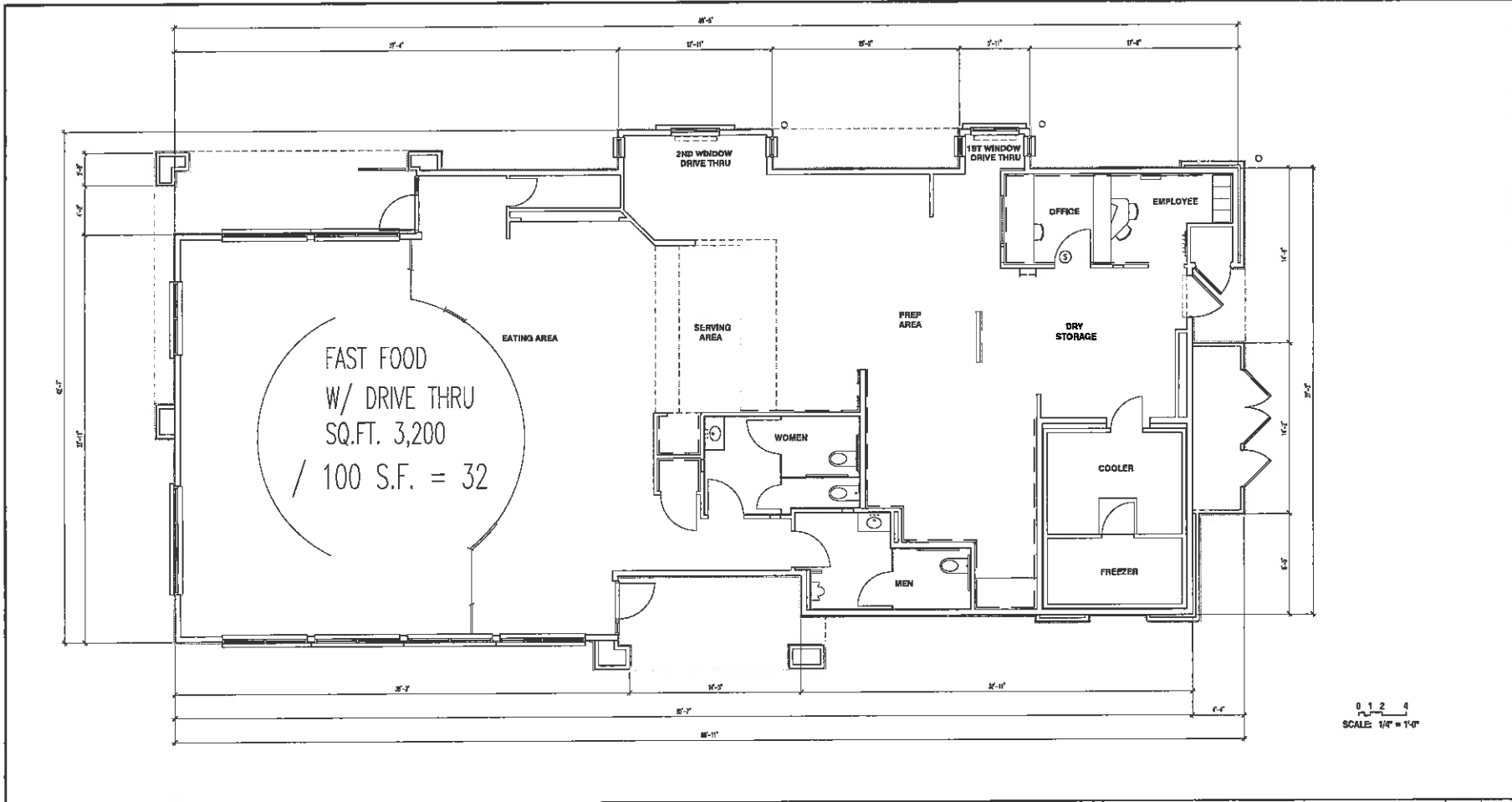


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Notes



224 SOUTH JOY STREET
CORONA, CA 92625-1928
U.S. 904.195.1958
CORONA, CA 92625-1958
DIRECT (951) 282-3832
FAX (951) 282-3832

**FASTFOOD W/ DRIVE THRU
#2 AND #3 FLOOR PLAN**

MORENO VALLEY CACTUS
NEC CACTUS AE AND COMMERCE CTR
MORENO VALLEY, CA

0 1 2 4
SCALE: 1/4" = 1'-0"

FLOOR PLAN SHEET 1

EXTERIOR

1 SINGLE 2x6 STUDS AT 16" O.C.
SCALE: 1 1/2" = 1'-0"

1A DOUBLE 2x6 STUDS AT 24" O.C.
SCALE: 1 1/2" = 1'-0"

NOTE: FOR SHEATHING TYPE REFER TO EXTERIOR SHEATHING SCHEDULE "S12"

EXTERIOR

2 1 1/2" METAL STUD EXTERIOR WALL
SCALE: 1 1/2" = 1'-0"

NOTE: FOR SHEATHING TYPE REFER TO EXTERIOR SHEATHING SCHEDULE "S12"

ALTERNATE:

3 TYPICAL STUD WALL AT 16" O.C.
SCALE: 1 1/2" = 1'-0"

3A STUD WALL WITH SOUNDBATT
SCALE: 1 1/2" = 1'-0"

3B STUD WALL AT 12" O.C.
SCALE: 1 1/2" = 1'-0"

NOTE: OPTION TO USE (3) 1/4" LAYERS OF GYPSUM BOARD FOR RADIUS BOTH SIDES

GENERAL NOTES:

- DIMENSION LINES ARE FROM FACE OF FRAMING OR FOUNDATION UNLESS NOTED OTHERWISE.
- FACE OF FRAMING ON EXTERIOR WALL IS SAME AS OUTSIDE FACE OF FOUNDATION WALL.
- METAL STUDS MAY BE USED IN PLACE OF WOOD STUDS. PLAN DIMENSIONS ARE BASED ON WOOD STUDS.
- MOISTURE RESISTANT GYPSUM TO BE USED IN TOILET ROOMS, BEHIND WOP SINK, AND ADJACENT TO COOLER FOR SPECIFIC SUBSTRATE REQUIREMENTS, REFER TO INTERIOR ELEVATIONS

KEYED NOTES:

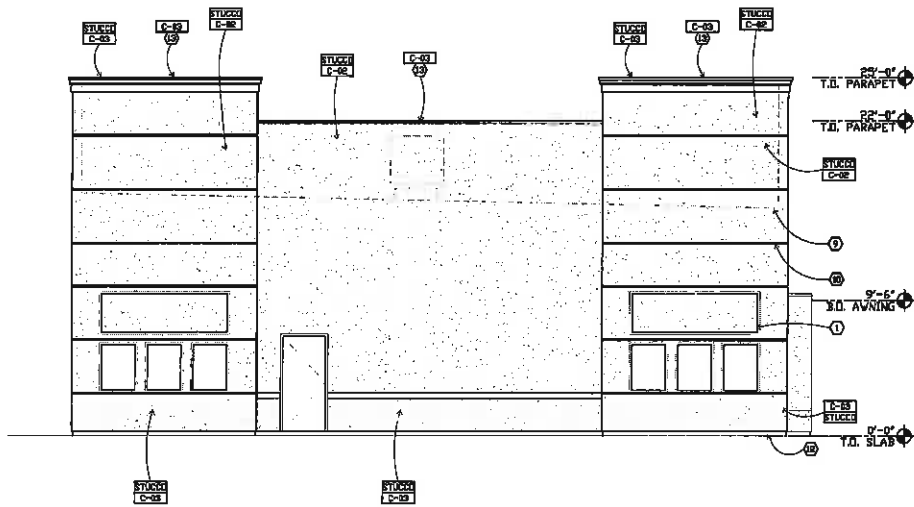
- FOR COOLER AND FREEZER INFORMATION AND DIMENSIONS, REFER TO SHEET 04.1
- COOLER WALL / BULKHEAD CONSTRUCTION TO BOTTOM OF STRUCTURE
- LINE OF PARAPET WALL ABOVE
- CUTTER ABOVE
- FINISH ABOVE
- LOW WALL ROUGH FRAMING HEIGHT = 5'-0", PROVIDE 5/8" MOISTURE RESISTANT GYPSUM BOARD BEHIND WOP SINK
- MAINTAIN MIN. 1" MIN. AIR GAP BEHIND COOLER AND FREEZER. CLOSE GAP AT ENDS WITH 2"x2" ALUMINUM ANGLE.
- BUILDING SIGN BY SIGN CONTRACTOR
- 3" CONCRETE SLAB FOR ELECTRICAL EQUIPMENT
- CLEAR PLASTIC COVER GUNNS

WALL LEGEND

- WOOD FRAMED WALL
- METAL STUD WALL
- MASONRY WALL
- PREFABRICATED COOLER UNIT
- LOW (1/2") WALL (WOOD FRAMED)

REVISION	DATE	BY

DATE	JOB NO
05-03-15	000.000
SCALE	DRAWN BY
AS SHOWN	IND
XXXXX	
DRAWING NO.	
A1-2	



EAST ELEVATION

SCALE: 1/4" = 1'-0" 3

GENERAL NOTES:

A. REVEAL LOCATIONS SHOWN ARE TO BE MATCHED AS CLOSELY AS POSSIBLE. SOME VARIATION DUE TO PRODUCT LIMITATIONS WILL BE ACCEPTABLE.

KEYED NOTES:

- ① SQUARE BY SIGN CONTRACTOR.
- ② ALUMINUM SEVENTH STORY SYSTEM.
- ③ EXTERIOR LIGHT.
- ④ ROOF TOP MECHANICAL UNITS.
- ⑤ GALVANIZED STEEL COLLECTION BIN.
- ⑥ L.E.D. LASER LIGHT IN PLASTIC TUBE TO MATCH COLOR.
- ⑦ COPING TO CORNER DOWN FACE OF WALL.
- ⑧ 4" DIA. DOWNPOUT BY METAL-DEKA OR EQUAL.
- ⑨ APPROXIMATE ROOF LINE.
- ⑩ 1" REVEALS BY FINISH.
- ⑪ FINISH FLOOR AND FRAME AS NOTED.
- ⑫ 4" BREAK METAL BASE.
- ⑬ PREFABRICATED COPING CAP.
- ⑭ EXTERIOR MOUNTED ELECTRICAL EQUIPMENT, REFER TO ELECTRICAL SHEETS.
- ⑮ NOT USED.
- ⑯ 30"x30" OPENING IN TOWER WALL (BEYOND).
- ⑰ EXTERIOR ROOF ACCESS LADDER, ALACO (#30 (ALL WALL MOUNT) BY SECURITY DOWN FLOOR AND DOUBLE CHAIN).
- ⑱ 4" CONCRETE CURB.
- ⑲ CONCRETE CURB AND CONCRETE WALK.
- ⑳ 6" ROAD CONCRETE FILLED STEEL WOLLAND.

EXTERIOR FINISHES

FC-1 PIGMENTED OPAQUE PLASTIC OR FORMED ALUMINUM BULLDOZE WITH TROUGH FOR L.E.D. AND FLAT RETURN

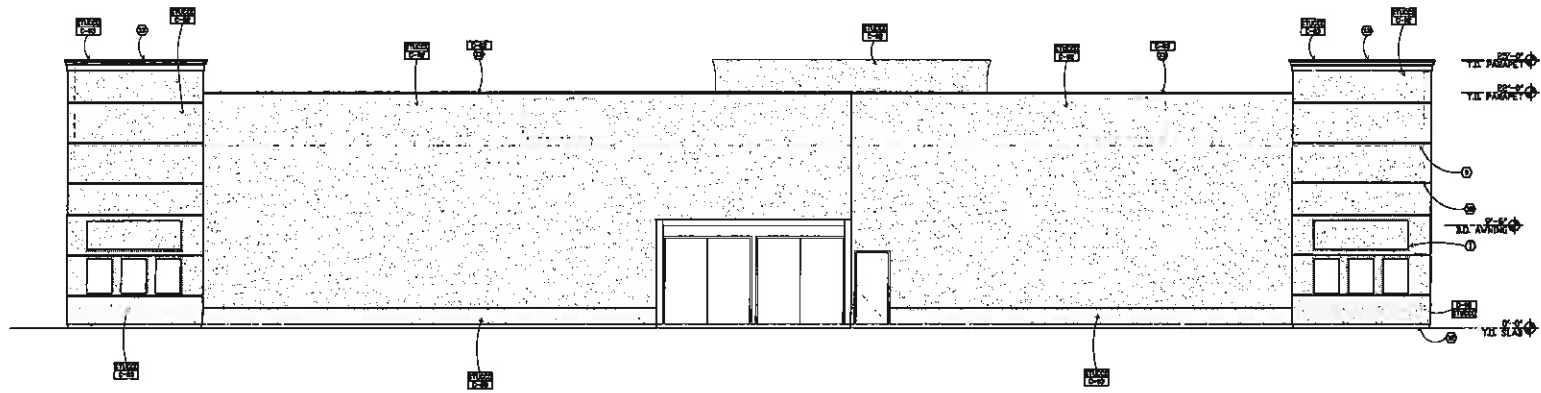
STUCCO 7/8" STUCCO (PORTLAND CEMENT PLASTER) OVER METAL LATH PROVIDE CONTROL JOISTS AS REQUIRED

EXTERIOR PAINTS

C-51 COLOR: CLEAR ANODIZED ALUMINUM FINISH

C-52 COLOR: OP PEARL-SEMI-GLOSS

C-53 COLOR: DEEP PEARL-SEMI-GLOSS



NORTH ELEVATION

SCALE: 1/8" = 1'-0" 4

224 SOUTH 10TH STREET
CORONA, CA 92629-1954
P.O. BOX 1038
CORONA, CA 92629-1958
DIRECT (951) 260-3833
FAX (951) 260-3832



CONVENIENCE STORE AND
FASTFOOD #1 ELEVATIONS
MORENO VALLEY CACTUS
NEC CACTUS AVE AND COMMERCE CTR
MORENO VALLEY CA

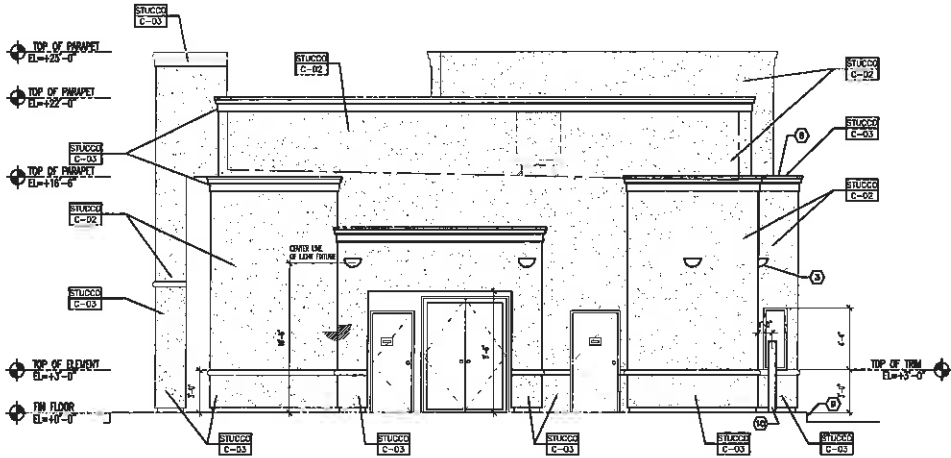
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ELEVATION B

SCALE: 3/4" = 1'-0" 3

GENERAL NOTES:

1. REVEAL LOCATIONS SHOWN ARE TO BE MATCHED AS CLOSELY AS POSSIBLE. SOME VARIATION DUE TO PRODUCT LIMITATIONS WILL BE ACCEPTABLE.

KEYED NOTES:

- 1) SOURCE BY BOB CONTRACTOR.
- 2) ALUMINUM STOREFRONT SYSTEM.
- 3) EXTERIOR LIGHT.
- 4) ROOF TOP MECHANICAL UNITS.
- 5) COPING TO EXTEND DOWN FACE OF WALL.
- 6) APPROXIMATE ROOF LINE.
- 7) 4" BREAK METAL BASE.
- 8) PREMANUFACTURED COPING CAP COLOR: DEEP PEARL, 13 4216-0000 - SEMI-GLOSS.
- 9) 6" CONCRETE CURB.
- 10) 6" ROUND CONCRETE FILLED STEEL BOLLARD.
- 11) BICYCLE RACK.

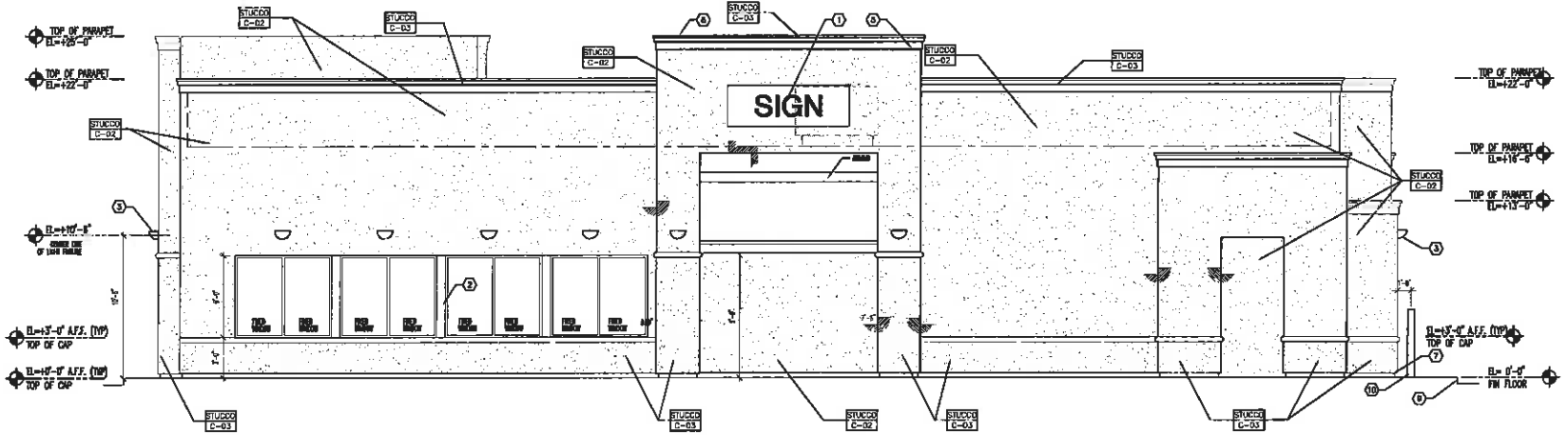
EXTERIOR FINISHES

EX-1 FIBERGLASS OPALQUE PLASTIC OR FORMED ALUMINUM BOLLARD WITH TROUGH FOR L.E.S. AND FLAT RETURN.

STUCCO 7/8" STUCCO (PORTLAND CEMENT PLASTER) OVER METAL LATH. PROVIDE CONTROL JOINTS AS REQUIRED.

EXTERIOR PAINTS

- C-01** COLOR: CLEAR ANODIZED ALUMINUM FINISH
- C-02** COLOR: EP PEARL-SEMI-GLOSS
- C-03** COLOR: DEEP PEARL-SEMI-GLOSS



ELEVATION A

SCALE: 3/4" = 1'-0" 4

228 SOUTH 8TH STREET
CORONA, CA 92673-1858
P.O. BOX 1058
CORONA, CA 92678-1058
DIRECT (951) 280-3853
FAX (951) 280-3852



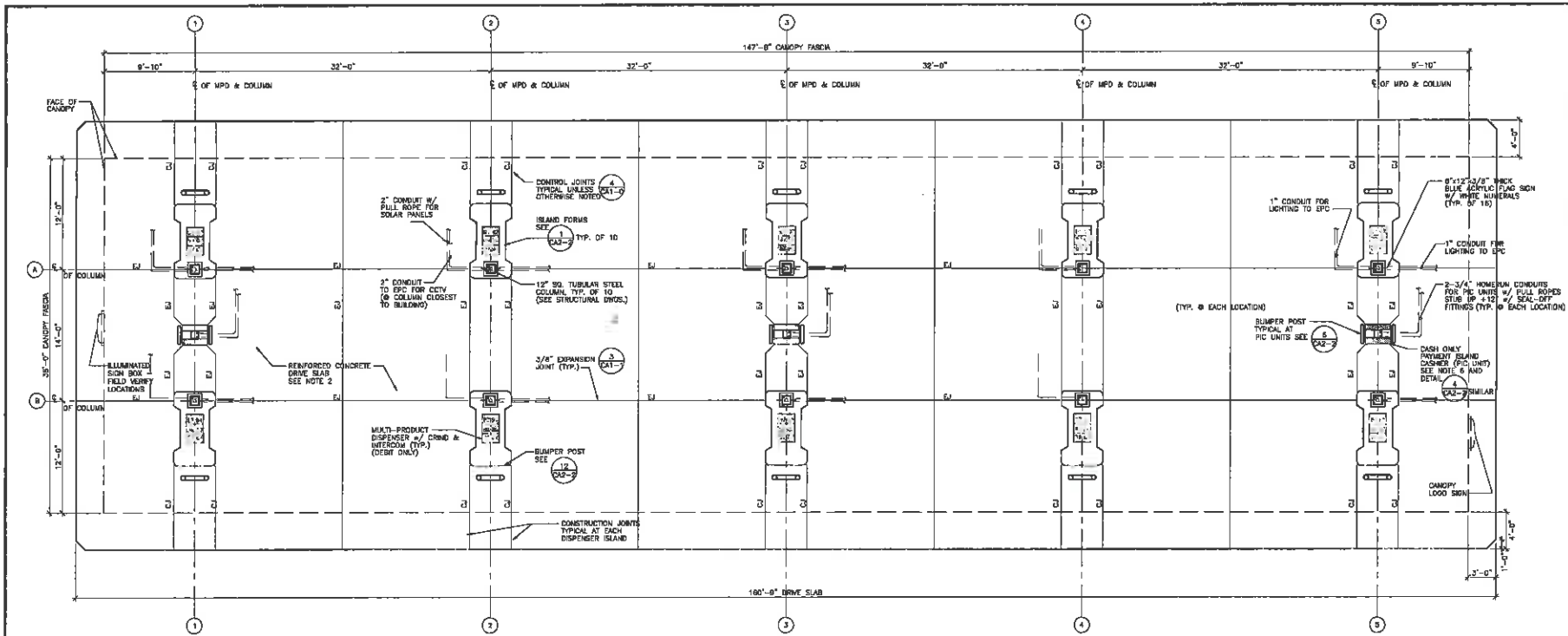
FAST FOOD W/ DRIVE THRU #2 AND #3 EXTERIOR ELEVATIONS
MORENO VALLEY CACTUS
NEC CACTUS AVE AND COMMERCE CTR
MORENO VALLEY, CA

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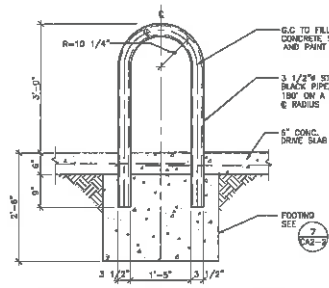
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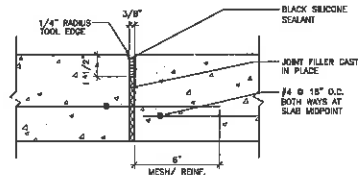
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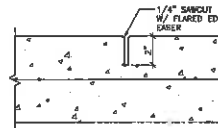
01 CANOPY SLAB PLAN
SCALE 3/16" = 1'-0"



2 PIC Bumper Post Detail
SCALE 3/4" = 1'-0"



3 Expansion Joint
SCALE 3" = 1'-0"



4 Control Joint
SCALE 3" = 1'-0"

Notes:

- DOWNPOINTS ARE DESIGNED TO BE TIGHTLINED TO THE SITE'S UNDERGROUND STORM DRAINAGE SYSTEM.
- DRIVE SLAB SHALL BE 8" THICK REINFORCED CONCRETE W/ #4 @ 18" O.C. EACH WAY OVER 4" CRUSHED ROCK OR AS SPECIFIED IN SOILS REPORT. FINISH SHALL BE A MEDIUM BROOM PERPENDICULAR TO ISLAND LAYOUT W/ EXPANSION AND CONTROL JOINTS AS SHOWN.
- CANOPY CONTRACTOR SHALL PROVIDE CONDUITS AND CONDUIT FITTINGS FOR LIGHTING, SIGN, CCTV, SOLAR AND SATELLITE IN COLUMNS WHERE SPECIFIED. COORDINATE WITH GENERAL CONTRACTOR FOR EXACT LOCATION OF CONDUITS, AND HAND HOLES PRIOR TO FABRICATION.
- EACH PAIR OF ISLANDS HEIGHTS SHALL BE THE SAME.
- ISLAND FORMS SHALL BE PAINTED BLACK.
- PAYMENT ISLAND CASHIER (P.I.C.) UNITS ARE BASED ON GEN III FRONT LOAD DOUBLE SIDED UNITS. CASH ACCEPTING UNITS ONLY.
- SEE SHEET CA1-2 AND ELECTRICAL DRAWINGS FOR ADDITIONAL NOTES AND IN COLUMN ELECTRICAL CONDUIT INFORMATION.
- CHALK BREAKS AT CONCRETE DRIVE SLAB SHALL OCCUR WITHIN DRUPLINE OF CANOPY.

224 SOUTH 300 STREET
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P.O. BOX 1968
CORONA, CA 92675-1959
DIRECT (951) 262-5832
FAX (951) 262-5832



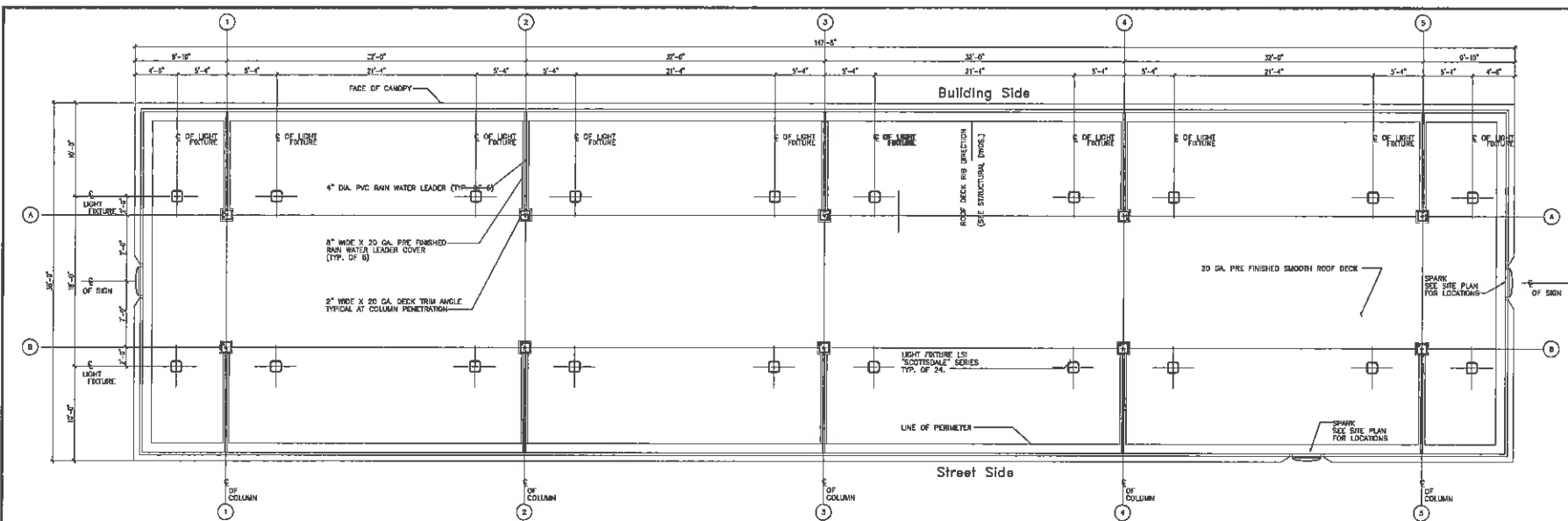
10 MPD CANOPY
SLAB PLAN
MORENO VALLEY CACTUS
NEC CACTUS AVE AND COMMERCE CTR
MORENO VALLEY, CA

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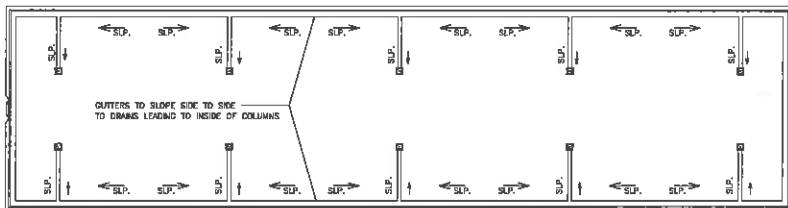
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PLOT: MORENO VALLEY_CACTUS_MPDCANOPY.CAD:1:MPD CANOPY SLAB PLAN: 06-25-15 10:00:05



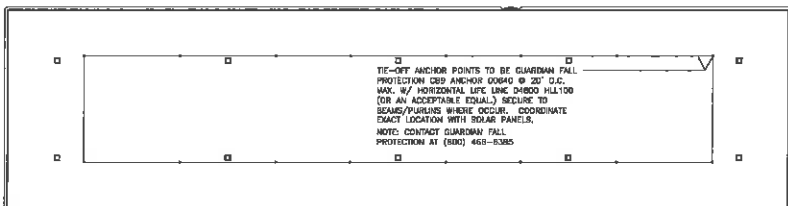
Canopy Reflected Ceiling Plan

SCALE: 3/16" = 1'-0"



ROOF PLAN

SCALE: 1/16" = 1'-0"



Safety System Plan

SCALE: 1/16" = 1'-0"

Schedule of In-Column Requirements by Canopy Mfr.:

1-A	3/4" DOWNSPOUT	1" CONDUIT FOR SECURITY SYSTEM
2-A	3/4" DOWNSPOUT	1" CONDUIT FOR LIGHTING
3-A	3/4" DOWNSPOUT	1" CONDUIT FOR SECURITY SYSTEM
		1" CONDUIT FOR FUTURE SATELLITE
4-A	3/4" DOWNSPOUT	1" CONDUIT FOR LIGHTING
5-A	3/4" DOWNSPOUT	1" CONDUIT FOR LIGHTING
1-B	3/4" DOWNSPOUT	1" CONDUIT FOR FUTURE
2-B	3/4" DOWNSPOUT	
3-B	3/4" DOWNSPOUT	1" CONDUIT FOR SPARE
4-B	3/4" DOWNSPOUT	1" CONDUIT FOR LIGHTING
5-B	3/4" DOWNSPOUT	1" CONDUIT FOR LIGHTING

ALL COLUMNS TO HAVE 1" CONDUIT FOR CCTV CAMERA DROPS PER DETAIL

Canopy Ceiling and General Notes

- ALL CEILING DECK SHALL BE 20 GA. EMBOSSED AND PREFERRED WHITE ROLLED PANS WITH 3-INCH HIGH RIBS AT 18 INCHES O.C. PANELS SHALL BE INTERLOCK INTERLOCKING SECTIONS CLIPPED TO CANOPY FRAME PER STRUCTURAL DINGS. OR PER MANUFACTURER'S REQUIREMENTS.
- ALL EXPOSED METAL SURFACES UNDER CANOPY, INCLUDING BUT NOT LIMITED TO, CORNICES, RAINWATER LEADER COVERS AND COLUMN TRIM SHALL BE FIRE FINISHED TO MATCH CEILING DECK.
- CANOPY CONTRACTOR SHALL PROVIDE OPENINGS IN DECK FOR LIGHT FIXTURES WHICH SHALL BE FURNISHED BY OWNER AND INSTALLED BY GENERAL CONTRACTOR.
- FASCIA GRAPHICS BACKING AND FLASHING SHALL BE PROVIDED BY CANOPY CONTRACTOR. ARCH GRAPHICS, BLUE TIRM SPARK ETD ARE PROVIDED BY ARCH'S GRAPHICS CONTRACTOR UNDER SEPARATE CONTRACT UNLESS CANOPY AND GRAPHICS CONTRACTOR ARE THE SAME ENTITY.
- CANOPY CONTRACTOR IS RESPONSIBLE FOR ALL DRAINAGE AND OVERFLOW FROM CANOPY ROOF INCLUDING, BUT NOT LIMITED TO, CUTTERS, RAINWATER LEADERS, OVERFLOWS, DOWNSPOUTS AND SUITABLE CONNECTION STUBS AT COLUMN BASE WHERE O.C. SHALL THEN EXTEND TO THE SITE'S DRAINAGE SYSTEM. SEE CA-1 NOTE #1.
- THE CANOPY SHALL BE ERRECTED IN TWO STAGES:
 - STAGE 1: CANOPY CONTRACTOR TO FRESH ANCHOR BOLTS AND TEMPLATES FOR GENERAL CONTRACTOR'S INSTALLATION PRIOR TO POURING FOUNDATIONS. WHEN FOUNDATIONS ARE SET, CANOPY CONTRACTOR SHALL INSTALL COLUMNS AND SECURE THEM TO FOUNDATIONS. COLUMNS SHALL BE STABLE, PLUMB AND TRUE.
 - STAGE 2: AFTER DRIVE SLAB AND SURROUNDING GRADE IS EITHER PAVED OR STABLE BASE HAS BEEN INSTALLED, CANOPY CONTRACTOR SHALL COME BACK TO THE SITE TO COMPLETE THE CANOPY. CANOPY CONTRACTOR SHALL VERIFY THAT COLUMNS HAVE REMAINED PLUMB PRIOR TO COMMENCING CANOPY ROOF INSTALLATION.
- ALL CONDUIT INSTALLED BY CANOPY CONTRACTOR SHALL HAVE PULL BOXES AND SHALL BE ACCESSIBLE TO GENERAL CONTRACTOR'S ELECTRICIAN TO MAKE CONNECTIONS. EACH END OF CONDUIT SHALL BE TAGGED WITH IDENTIFYING OF CONDUIT RUN (I.E. SALES COUNTER TO CANOPY ROOF).

204 SOUTH 20TH STREET
CORONA, CA 92623-1558
CORONA, CA 92626-1288
DIRECT (951) 280-3823
FAX (951) 280-3822



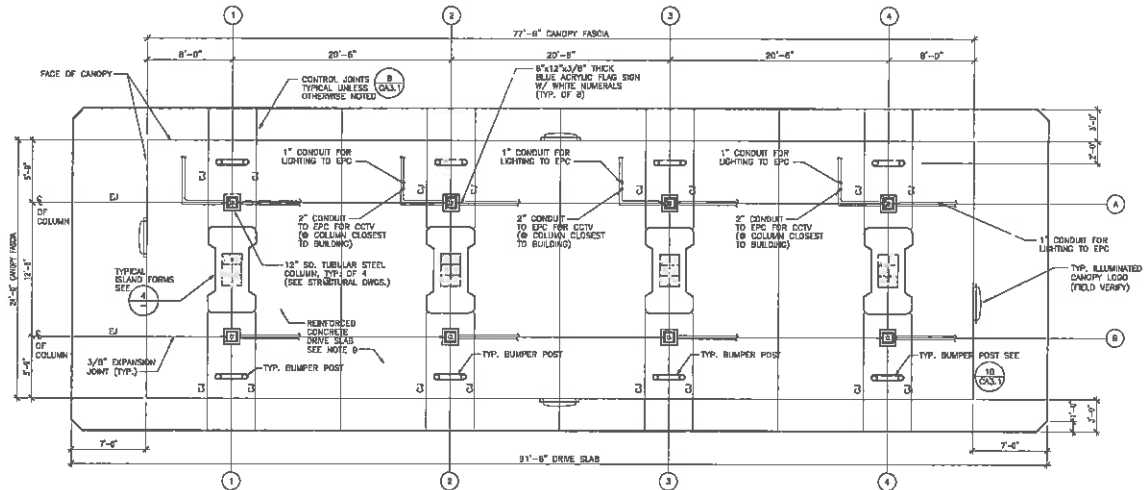
10 MPD CANOPY REFLECTED CEILING PLAN
MORENO VALLEY CACTUS AND COMMERCE CTR
MORENO VALLEY, CA



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1 Diesel Canopy Slab Plan
SCALE: 3/16" = 1'-0"

Schedule of In-Column Requirements by Mfr.:

A-1	3" DOWNSPOUT 1" CONDUIT FOR SECURITY SYSTEM
B-1	3" DOWNSPOUT 1" CONDUIT FOR LIGHTING
A-2	3" DOWNSPOUT 1" CONDUIT FOR FUTURE
B-2	3" DOWNSPOUT

COLUMN NOTE:
ALL COLUMNS TO HAVE
1" CONDUIT FOR
CCTV CAMERA DOORS

224 SOUTH OY STREET
CORONA, CA 92629-1959
P.O. BOX 1818
CORONA, CA 92624-1898
DIRECT (951) 260-3833
FAX (951) 260-3832



CANOPY DIESEL
SLAB PLAN

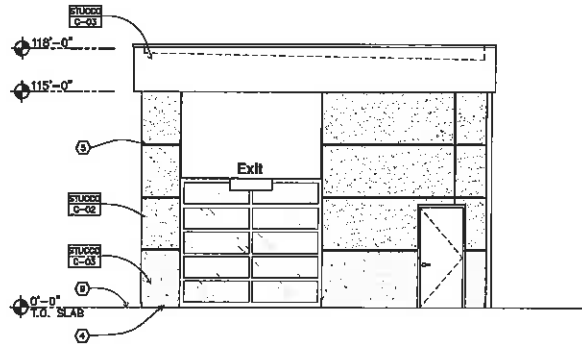
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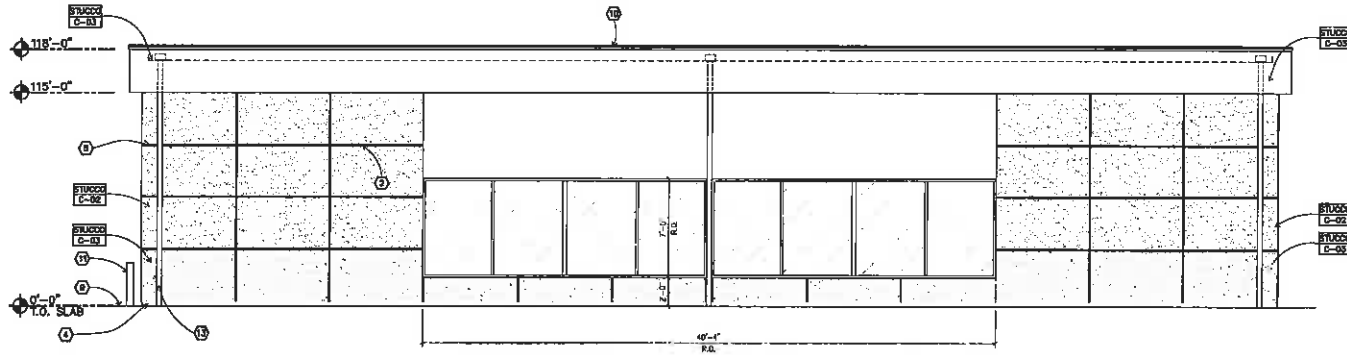
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SOUTH ELEVATION

SCALE: 1/4" = 1'-0" 1



EAST ELEVATION

SCALE: 1/4" = 1'-0" 2

KEYED NOTES:

- 1 VINYL LETTERS APPLIED TO ACM PANEL.
- 2 1" REVEAL, EACH SIDE.
- 3 PROMOTED OPACQUE PLASTIC BULLNOSE.
- 4 1" BREAK METAL BARKLEXTEND TO 10" WHERE SHOWN.
- 5 1" REVEAL IN FINISH (TYPICAL).
- 6 NO ENTRY - 10" x 30" SIGN CENTERED OUTSIDE OF OVERHEAD DOOR.
- 7 OVERHEAD CLEARANCE BAR PROVIDED BY OWNER, VERIFY MOUNTING HEIGHT ABOVE APPROX. 8'6" OVER.
- 8 INSTRUCTIONAL SIGN PANEL.
- 9 CONCRETE EXTENDED CURB AND CONCRETE WALK.
- 10 PREFABRICATED HARBLEBLE ROOF.
- 11 4" ROUND CONCRETE FILLED STEEL BOLLARD, REFER TO DETAIL 02-011.
- 12 HOLLOW METAL DOOR AND FRAME, PAINT BOTH AS NOTED.
- 13 3/4" GALVANIZED STEEL DOWNSPOUT.

EXTERIOR FINISHES

- FLUCCO-1 PROMOTED OPACQUE PLASTIC OR FINISH ALUMINUM BULLNOSE WITH TROUGH FOR L.E.D. AND FLAT RETURN.
- FLUCCO-2 7/8" SLICED (PORTLAND CEMENT PLASTER) OVER METAL LATH PROVIDE CONTROL JOINTS AS REQUIRED.

EXTERIOR PAINTS

- C-01 COLOR: CLEAR ANODIZED ALUMINUM FINISH
- C-02 COLOR: BP PEARL-SSA-GLOSS
- C-03 COLOR: DEEP PEARL-SEMI-GLOSS

224 SOUTH JOY STREET
CORONA, CA 92709-1559
P.O. BOX 1558
CORONA, CA 92702-1558
DIRECT (951) 280-3893
FAX (951) 280-3892



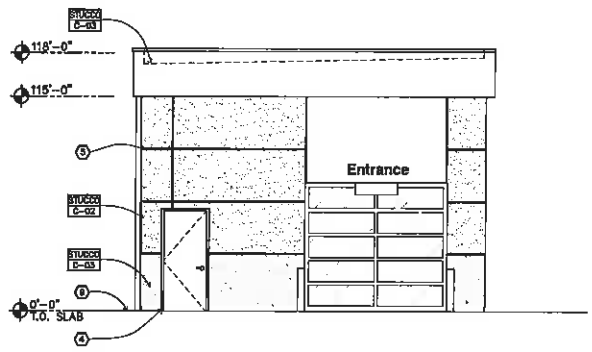
CAR WASH
EXTERIOR ELEVATIONS
MORENO VALLEY CACTUS
AND COMMERCE CTR
MORENO VALLEY, CA

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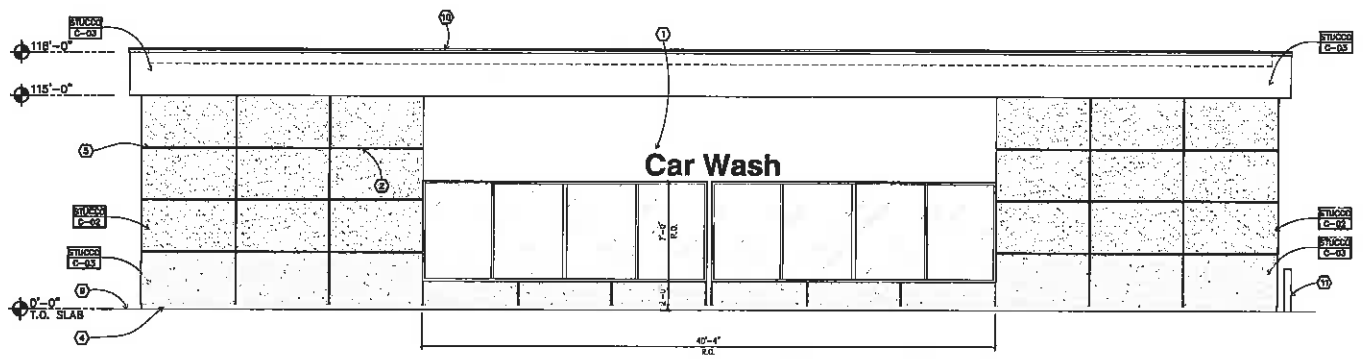
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4/27/15/Moreno Valley Cactus and Commerce CTR - 15-11-10 100002



NORTH ELEVATION

SCALE: 3/4" = 1'-0" 3



WEST ELEVATION

SCALE: 3/4" = 1'-0" 4

KEYED NOTES:

- 1 VINYL LETTERS APPLIED TO SIGN PANEL.
- 2 1" REVEAL EACH SIDE.
- 3 PAINTED OPaque PLASTIC BALLNOSE
- 4 4" BREAK METAL BASE EXTEND TO 90° WHERE SHOWN
- 5 1" REVEAL IN FRONT (TYPICAL)
- 6 NO OVERHANG - 1/2" x 3/8" SIGN CENTERED OUTSIDE OF OVERHEAD DOOR.
- 7 OVERHEAD CLEARANCE BAR PROMOTED BY OWNER. VERIFY MOUNTING HEIGHT ABOVE APPROX. WITH OWNER.
- 8 INSTRUCTIONAL SIGN PANEL.
- 9 CONCRETE EXPOSED CURBS AND CONCRETE WALK
- 10 PREFABRICATED INALIABLE ROOF
- 11 6" ROUND CONCRETE FILLED STEEL BOLLARD, REFER TO DETAIL 02-511
- 12 HOLLOW METAL DOOR AND FRAME, PAINT BOTH AS NOTED
- 13 2 1/2" GALVANIZED STEEL DOWNSPOUT

EXTERIOR FINISHES

- PL-1 PAINTED OPaque PLASTIC OR FORMED ALUMINUM BALLNOSE WITH BRUSH FOR L.C.D. AND FLAT RETURN
- STUCCO 7/8" STUCCO (PORTLAND CEMENT PLASTER) OVER METAL LATH. PROTECT JOINTS AS REQUIRED

EXTERIOR PAINTS

- E-01 COLOR: CLEAR ANODIZED ALUMINUM FINISH
- E-02 COLOR: EP PEARL-SEM-GLOSS
- E-03 COLOR: DEEP PEARL-SEM-GLOSS

224 SOUTH 2ND STREET
CORONA, CA 92707-1958
CORONA, CA 92707-1958
CORONA, CA 92707-1958
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CAR WASH
EXTERIOR ELEVATIONS
MORENO VALLEY CACTUS
NEC CACTUS AVE AND COMMERCE CTR
MORENO VALLEY, CA

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NOTICE OF PUBLIC HEARING

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Thursday, from 8:00 a.m. to 5:00 p.m., except Monday, September 7 (Labor Day).

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon St., 1st Floor Hearing Room
Riverside, California

DATE OF HEARING: September 10, 2015

TIME OF HEARING: 9:00 A.M.

CASE DESCRIPTION:

ZAP1135MA15 – Jack Kofdarali and Ino Cruz/French Valley Benton Rd., LP – City of Moreno Valley Case No.: PA15-0032 (Conditional Use Permit). The applicant proposes to develop and operate a 48,833 square foot industrial warehouse (including 5,446 square feet of office space), a gasoline and diesel service station with 14 pumps, a 3,800 square foot convenience store, three fast-food restaurants with a combined gross floor area of 9,200 square feet, and a car wash on 6.31 acres located northerly of Cactus Avenue, easterly of Commerce Center Drive, and southerly of Goldencrest Drive in the City of Moreno Valley. (Assessor's Parcel Numbers 297-130-052, -053, and -054) (Airport Compatibility Zone B2 of the March Air Reserve Base/Inland Port Airport Influence Area).

FURTHER INFORMATION: Contact Russell Brady at (951) 955-0549 or John Guerin at (951) 955-0982. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Mr. Gabriel Diaz of the City of Moreno Valley Planning Division, at (951) 413-3226.

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.3

HEARING DATE: September 10, 2015

CASE NUMBER: ZAP1033TH15 – Ross & Sons Refrigeration & Construction, for Anthony Vineyards, Inc. (Representative: Audrey Nickerson, The Altum Group)

APPROVING JURISDICTION: City of Coachella

JURISDICTION CASE NO: 15-05 (Architectural Review)

MAJOR ISSUES: None.

RECOMMENDATION: Staff recommends that the proposed project be found **CONSISTENT** with the 2005 Jacqueline Cochran Regional Airport Land Use Compatibility Plan, as amended in 2006, subject to the conditions included herein.

PROJECT DESCRIPTION: The landowner proposes to construct a new 33,560 square foot freezer (cold storage) building and a 14,816 square foot open canopy to supplement the existing cold storage building and canopy at the Anthony Vineyards facility located on 8.48 acres.

PROJECT LOCATION: The project site is located at the northwest corner of Enterprise Way and Industrial Way in the City of Coachella, approximately 10,450 feet northerly of the northerly terminus of Runway 17-35 at Jacqueline Cochran Regional Airport.

LAND USE PLAN: 2005 Jacqueline Cochran Regional Airport Land Use Compatibility Plan (last amended in 2006)

- a. Airport Influence Area: Jacqueline Cochran Regional Airport
- b. Land Use Policy: Airport Compatibility Zone C
- c. Noise Levels: Below 55 CNEL

BACKGROUND:

Average Intensity: The project site is located within Airport Compatibility Zone C. Non-Residential intensity in Airport Compatibility Zone C is restricted to 75 people per average acre. Based on the floor plans provided and the Building Code Method, the 33,560 square foot cold storage building would be considered to have a maximum occupancy of one person per 300 square feet for a total occupancy of 112 persons.

Existing buildings on the site include a 67,000 square foot cold storage building and an 8,526 square foot office. If one assumes a maximum occupancy of one person per 300 square feet for the storage building and one person per 100 square feet for the office building, this would indicate a total

occupancy of existing buildings of 308 persons. One would then assume a maximum of 420 persons on-site counting both existing and new buildings.

A total occupancy of 420 persons on an 8.48-acre site results in an average occupancy of 50 persons per acre, which is consistent with the maximum average intensity of 75 persons per acre within Airport Compatibility Zone C.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per standard vehicle and 1.0 person per truck parking or dock). Based on the number of parking spaces provided (138 standard vehicle [including 4 handicapped] spaces and 10 truck dock spaces), the total occupancy would be estimated at 217 persons for an average acre intensity of approximately 26 persons per acre, which is also consistent with Compatibility Zone C average acre intensity criteria.

Finally, it should be noted that the business owner anticipates a maximum of 45 persons at the site during the table grape receiving/shipping season in this area (May and June) and a maximum of 15 persons on the site at other times. These numbers would seem to indicate that the amount of parking being provided is excessive; however, the number of parking spaces being provided is required by City ordinance.

Single-Acre Intensity: The project is located within Airport Compatibility Zone C. Non-Residential intensity in Airport Compatibility Zone C is restricted to 150 people per single-acre. This limitation pertains to single-acre areas proposed for new construction or changes in use. The most intense single-acre area would consist of 43,560 square feet included in portions of the existing and proposed cold storage buildings. At an occupancy level of one person per 300 square feet, this area could accommodate a maximum of 145 persons, which would be consistent with the Compatibility Zone C single-acre maximum intensity criterion of 150 persons.

Prohibited Uses: The proposed use is not prohibited within Compatibility Zone C. None of the prohibited uses, as listed in Condition No. 2 below, are proposed in this project.

Noise: The project area is located outside the area subject to average aircraft noise above 55 CNEL. As such, no special measures to mitigate aircraft-generated noise are required.

PART 77: The elevation at the northerly end of Runway 17-35 at Jacqueline Cochran Regional Airport is -114 feet above mean sea level (AMSL). At a distance of approximately 10,450 feet from the runway to the property line, a building elevation at top of roof exceeding -10 feet AMSL would require FAA notice and review through the Form 7460-1 process. The project has an existing grade elevation of approximately -87 feet AMSL. The maximum height of the building proposed is 32 feet for a maximum elevation of approximately -55 feet AMSL. Therefore, review pursuant to the Federal Aviation Administration Obstruction Evaluation Service Form 7460-1 process for height/elevation reasons is not required.

Open Area: Compatibility Zone C requires 20% of area within major projects (10 acres or larger) to be set aside as open land that could potentially serve as emergency landing areas. The net area of the properties included in the project is 8.48 acres.

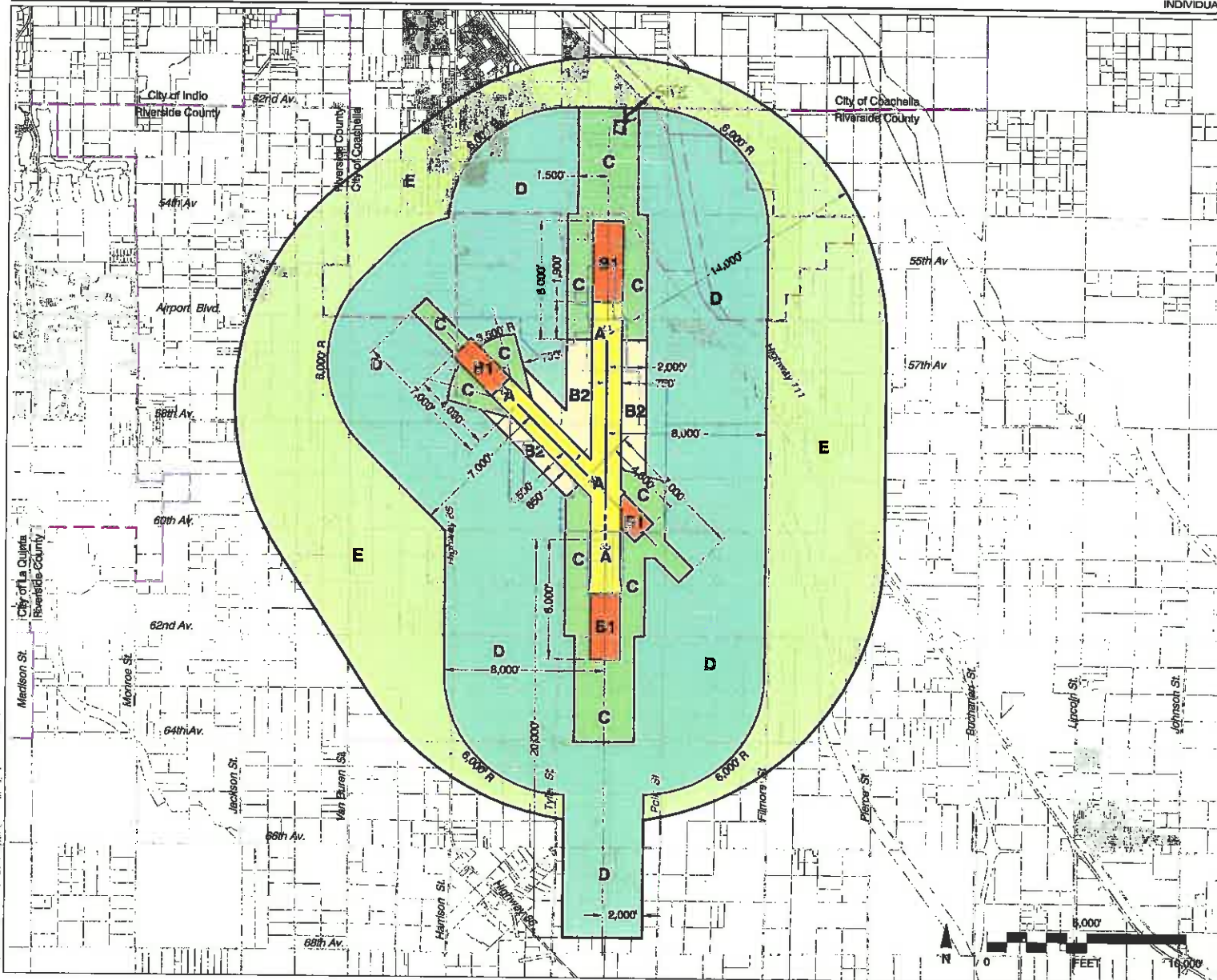
CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting plans, if any, shall be transmitted to Riverside County Economic Development Agency – Aviation Division personnel and to the Jacqueline Cochran Regional Airport for review and comment. (Failure to comment within thirty days shall be considered to constitute acceptability on the part of the airport manager.)
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, livestock operations, production of cereal grains, sunflower, and row crops, artificial marshes, wastewater management facilities, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children’s schools, day care centers, libraries, hospitals, nursing homes, highly noise-sensitive outdoor non-residential uses, and hazards to flight.
3. The attached notice shall be provided to all prospective purchasers of the property and tenants of the buildings thereon, and shall be recorded as a deed notice.
4. Any new detention basin(s) shall be designed so as to provide a maximum 48-hour detention period for the design storm (may be less, but not more), and to remain totally dry between

rainfalls. Vegetation in and around the detention basin that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Legend

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E

Boundary Lines

- Airport Property Line - Existing
- +— Airport Property Line - Planned
- City Limits

Note

Except for southern extension, Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

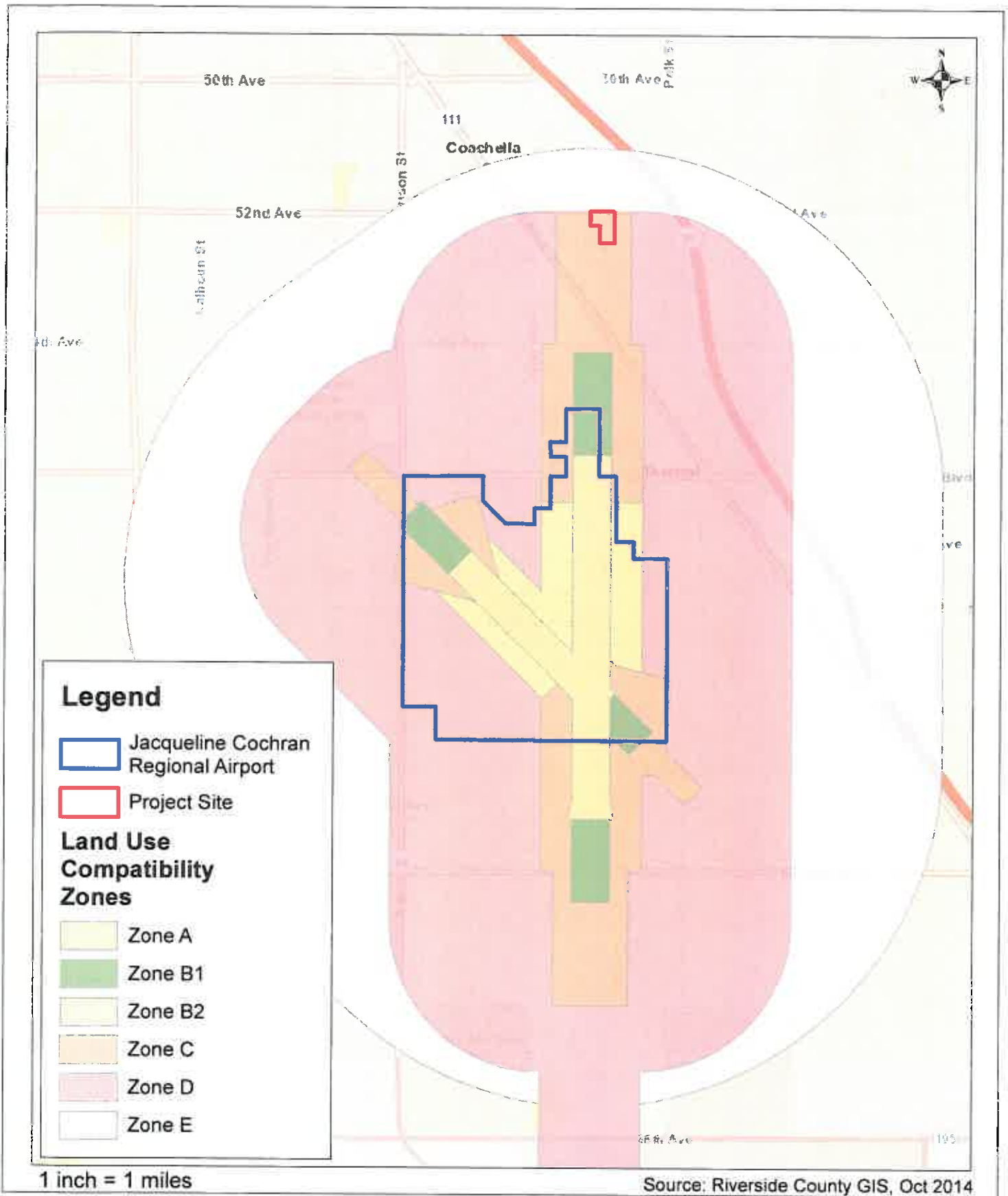
See Chapter 2, Table 2A for compatibility criteria associated with this map.

Riverside County
 Airport Land Use Commission
**Riverside County
 Airport Land Use Compatibility Plan
 Policy Document**
 (Adapted June 2005)

Map JC-1

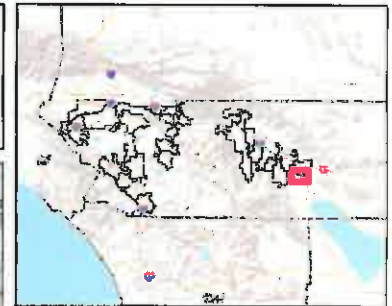
Compatibility Map
 Jacqueline Cochran Regional Airport





Jacqueline Cochran Airport Compatibility Zones
 Anthony Vineyards ALUC Review

My Map



Legend

- City Boundaries
- highways
 - HWY
 - INTERCHANGE
 - INTERSTATE
 - OFFRAMP
 - ONRAMP
 - USHWY
- majorroads
- counties
- cities
- hydrographylines
- waterbodies
 - Lakes
 - Rivers



0 4,531 9,062 Feet



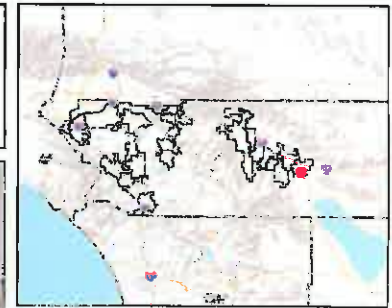
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 8/26/2015 12:40:19 PM

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Notes

My Map



Legend

- RCLIS Parcels
- City Boundaries
- roadsanno
- highways
 - HWY
 - INTERCHANGE
 - INTERSTATE
 - OFFRAMP
 - ONRAMP
 - USHWY
- counties
- cities
- hydrographylines
- waterbodies
 - Lakes
 - Rivers



0 1,133 2,266 Feet



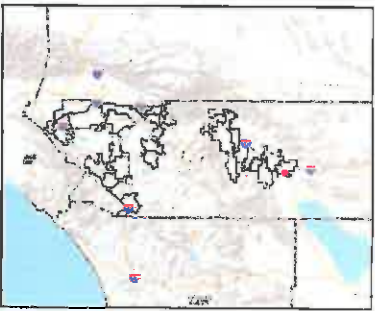
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Notes

My Map



Legend

- RCLIS Parcels
- City Boundaries
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
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- waterbodies
- Lakes
- Rivers



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Notes

0 283 566 Feet





Anthony Vineyards Freezer Building Expansion
Anthony Vineyards ALUC Review

NOTICE OF PUBLIC HEARING

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Thursday, from 8:00 a.m. to 5:00 p.m., except Monday, September 7 (Labor Day).

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon St., 1st Floor Hearing Room
Riverside, California

DATE OF HEARING: September 10, 2015

TIME OF HEARING: 9:00 A.M.

CASE DESCRIPTION:

ZAP1033TH15 – Ross & Sons Refrigeration & Construction, for Anthony Vineyards, Inc. (Representative: Audrey Nickerson, The Altum Group) – City of Coachella Architectural Review Case No. 15-05. The landowner proposes to construct a new 33,560 square foot freezer (cold storage) building and a 14,816 square foot open canopy to supplement the existing cold storage building and canopy at the Anthony Vineyards facility located on 8.48 acres at the northwest corner of Enterprise Way and Industrial Way. (Airport Compatibility Zone C of the Jacqueline Cochran Regional Airport Influence Area)

FURTHER INFORMATION: Contact Russell Brady at (951) 955-0549 or John Guerin at (951) 955-0982. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Mr. Luis Lopez of the City of Coachella Community Development Department, at (760) 398-3102.

APPLICATION FOR MAJOR LAND USE ACTION REVIEW
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

ALUC Identification No.

ZAP1033THIS

PROJECT PROPONENT (TO BE COMPLETED BY APPLICANT)

Date of Application _____

Property Owner Anthony Vineyards, Inc.

Phone Number (651)858-6211

Mailing Address 5512 Valpredo Avenue
Bakersfield, CA 93307

Agent (if any) Ross & Sons Refrigeration & Construction

Phone Number (559)696-4451

Mailing Address 7828 S Maple
Fresno, CA 93725

PROJECT LOCATION (TO BE COMPLETED BY APPLICANT)

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address 52-301 Enterprise Way
Coachella, CA 92236-2706

Assessor's Parcel No. 763-131-030, 763-131-031, 763-400-022

Parcel Size 8.53 AC (1.4 AC disturbed area)

Subdivision Name N/A

Lot Number N/A

Zoning Classification Heavy Industrial (M-H)

PROJECT DESCRIPTION (TO BE COMPLETED BY APPLICANT)

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe) Currently, Anthony Vineyard consists of an existing office, cold storage/warehouse, loading dock, parking lot, canopy, and outdoor storage. The perimeter of the site is fenced with a 6-foot chain link topped with barbed wire. The northern portion of the property is undeveloped and used as a date orchard.

Proposed Land Use (describe) The proposed project consists of the addition of a new freezer building (cold storage) and a new canopy to the existing fruit packing facility. It also includes a parcel merger to combine three parcels into one parcel, and recordation of a "no build" easement over a portion of four parcels onsite.

For Residential Uses Number of Parcels or Units on Site (exclude secondary units) _____

For Other Land Uses Hours of Use _____ 24 _____

(See Appendix C) Number of People on Site _____ Maximum Number _____ 45 (May/June); 15 (Rest of the Year)

Method of Calculation _____ Maximum increased during harvest

Height Data Height above Ground or Tallest Object (including antennas and trees) _____ 31.25 _____ ft.

Highest Elevation (above sea level) of Any Object or Terrain on Site _____ -55.75 _____ ft.

Flight Hazards Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight? Yes No

If yes, describe

*U.C
Zone C*

REFERRING AGENCY (APPLICANT OR JURISDICTION TO COMPLETE)	
Date Received	
Agency Name	City of Coachella
Staff Contact	Luis Lopez, Development Service Director
Phone Number	(760) 398-3102
Agency's Project No.	Architectural Review No. 15-05
Type of Project <input type="checkbox"/> General Plan Amendment <input type="checkbox"/> Zoning Amendment or Variance <input type="checkbox"/> Subdivision Approval <input type="checkbox"/> Use Permit <input type="checkbox"/> Public Facility <input checked="" type="checkbox"/> Other Site Plan Review, Parcel Merger, & Easement Recordation	

A. NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. SUBMISSION PACKAGE:

ALUC REVIEW

- 1 Completed Application Form
- 1 Project Site Plan – Folded (8-1/2 x 14 max.)
- 1 Elevations of Buildings - Folded
- 1 Each . 8 ½ x 11 reduced copy of the above
- 1 8 ½ x 11 reduced copy showing project in relationship to airport.
- 1 Set . Floor plans for non-residential projects
- 4 Sets. . Gummed address labels of the Owner and representative (*See Proponent*).
- 1 Set. . Gummed address labels of all property owners within a 300' radius of the project site. If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10), with ALUC return address.
- 4 Sets. . Gummed address labels of the referring agency (City or County).
- 1 Check for Fee (See Item "C" below)

STAFF REVIEW (Consult with ALUC staff planner as to whether project qualifies)

- 1 Completed Application Form
- 1 Project Site Plans – Folded (8-1/2 x 14 max.)
- 1 Elevations of Buildings - Folded
- 1 8 ½ x 11 Vicinity Map
- 1 Set . Gummed address labels of the Owner and representative (*See Proponent*).
- 1 Set . Gummed address labels of the referring agency.
- 1 Check for review—See Below



The Altum Group

73710 Fred Waring Dr., Ste. 219
Palm Desert, CA 92260
760.346.4750 Tel
760.340.0089 Fax

TRANSMITTAL

*Airport Land Use Commission
Received*

JUL 28 2015

To:	Barbara Santos	From:	Audrey Nickerson
Company:	Riverside County Airport Land Use Commission Riverside County Administrative Center	<input type="checkbox"/>	For Your Information
		<input checked="" type="checkbox"/>	For Your Approval
		<input checked="" type="checkbox"/>	For Your Review
		<input type="checkbox"/>	As Requested
Address:	4080 Lemon St., 14th floor Riverside, CA 92501		
Phone:	951-955-5132		
Date:	July 27, 2015		
File:	C1178 Anthony Vineyards		
Delivery:	The Altum Group		

Attached please find the following documents for the ALUC Review for the Anthony Vineyards Project:

- 1 ALUC Review Application Form
- 2 Project Site Plan (one copy 8 ½" x 14" & one copy 8 ½" x 11")
- 2 Elevations of Building (one copy 8 ½" x 14" & one copy 8 ½" x 11")
- 1 Floor Plans
- 1 Gummed Address Labels for Owners (4), Representatives (4 ea.), surrounding property owners (1 ea.), and referring agency (4)
- 1 Check for Fee



73710 Fred Waring Dr., Ste. 219
Palm Desert, CA 92260
760.346.4750 Tel
760.340.0089 Fax

The Altum Group

TRANSMITTAL

To:	Russell Brady	From:	Audrey Nickerson
Company:	Riverside County Airport Land Use Commission Riverside County Administrative Center	<input type="checkbox"/>	For Your Information
		<input checked="" type="checkbox"/>	For Your Approval
		<input checked="" type="checkbox"/>	For Your Review
		<input type="checkbox"/>	As Requested
Address:	4080 Lemon St., 14 th floor Riverside, CA 92501		
Phone:	951-955-0549		
Date:	August 4, 2015		
File:	C1178 Anthony Vineyards		
Delivery:	The Altum Group		

Attached please find the following documents for the ALUC Review for the Anthony Vineyards Project:

- 1 Project Site Plan (30x42" Color Copy)
- 1 Elevations of Building (30x42" Color Copy)

Guerin, John

From: Audrey Nickerson <audrey.nickerson@thealtumgroup.com>
Sent: Thursday, August 06, 2015 4:58 PM
To: Guerin, John
Subject: Anthony Vineyards ALUC Review_ Data Request

Good Afternoon John,

I had worked with Russell Brady on gathering some additional information he requested for the Anthony Vineyards ALUC Review. I had given him all the information except for a brief summary of operations for the site. Below is the information I received from the owners:

There are two distinct seasons of activity at our facility at 52-310 Enterprise Way, Coachella, Ca. First is the grape receiving/shipping season during May and June. Secondly is the balance of the year from July through April.

During table grape season the facility is receiving, cooling and shipping table grapes. Also we have our fruit salesmen on site. We have employees present from 6:00 AM and usually complete shipping by about midnight. Between 6:00 AM to 6:00 PM, we have maximum 45 on site. Between 6:00 PM to midnight there will be maximum 10 people on site.

During the non-grape season we have a small compliment of people in the office and have a staff of 6 to operate the cold storage and freezer facility. We have employees on site from 6:00 AM to 5:00 PM. Maximum would be 15 people at any one time.

Please let me know if there is any other information you need in order to complete the review.

Regards,

Audrey Nickerson
Planner



73-710 Fred Waring Drive, Suite 219
Palm Desert, CA 92260
p. 760.346.4750
f. 760.340.0089
Audrey.Nickerson@TheAltumGroup.com
www.TheAltumGroup.com

COUNTY OF RIVERSIDE AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM: 3.4
HEARING DATE: September 10, 2015
CASE NUMBER: ZAP1032TH15 – Allen Russak (Representative: Nick Fullerton, Fullerton Architects)
APPROVING JURISDICTION: County of Riverside
JURISDICTION CASE NO: BRS150586 (Building Permit)

MAJOR ISSUES: Peripheral glare potential

RECOMMENDATION: Staff recommends that the Commission make a finding of CONSISTENCY, subject to the conditions included herein.

PROJECT DESCRIPTION: The applicant proposes to install and operate roof mounted photovoltaic solar panels on a private member's seasonal residence/garage on Lot 202 (Assessor's Parcel Number 759-220-028) within the portion of the Thermal Club facility where overnight stays are allowed. This review is limited to the acceptability of roof mounted photovoltaic solar panels at this location.

PROJECT LOCATION: The project site is located at 61776 Fullerton Drive within the Thermal Club facility located northerly of 62nd Avenue, easterly of Tyler Street, southerly of 60th Avenue, and westerly of Polk Street in the unincorporated community of Thermal, approximately 4,664 feet southeasterly of the southerly terminus of Runway 17-35 at Jacqueline Cochran Regional Airport.

LAND USE PLAN: 2005 Jacqueline Cochran Regional Airport Land Use Compatibility Plan (last amended in 2006)

- a. Airport Influence Area: Jacqueline Cochran Regional Airport
- b. Land Use Policy: Airport Compatibility Zone D
- c. Noise Levels: Below 55 CNEL from aircraft at this location

BACKGROUND:

Prohibited Uses: Hazards to flight are a prohibited use in Airport Compatibility Zone D and, indeed, throughout the Airport Influence Area. Specifically, any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport is prohibited. Solar panels have the potential to create glare by reflecting sunlight. The applicant has prepared a glare analysis utilizing the web based Solar Glare Hazard Analysis Tool (SGHAT) and provided a report, a copy of which is attached hereto. The analysis was based on approach patterns to both runways 35 and 30 from the south and southeast, respectively. The analysis utilized a glide slope approach of 3.0

degrees and a 27 degree tilt southerly to the solar panels. The analysis concluded that some glare would occur on the approach to both runway 30 and 35. The glare that is projected to occur on the approach to runway 30 would be generally limited to less than ten minutes a day during the times of year at the selected distances from the runway:

2 mile

- March to April – 5:15 p.m. to 5:45 p.m. standard time (6:15 to 6:45 PDT)
- Mid September to Early October – 4:45 p.m. to 5:15 p.m. standard time (5:45 to 6:15 PDT)

The glare that is projected to occur on the approach to runway 35 would be generally limited to less than ten minutes a day during the times of year at the selected distances from the runway:

1 mile

- March to April – 6:00 a.m. to 7:00 a.m. (7:00 to 8:00 PDT)
- Mid September to Early October – 6:00 a.m. to 6:30 a.m. (7:00 to 7:30 PDT)

1 ¼ mile

- May to Early August – 6:30 a.m. to 7:30 a.m. (7:30 to 8:30 PDT)

No glare would occur closer than 1 mile from the runway. All glare would be beyond 50 degrees from pilot line-of-sight. The analysis notes that the glare experienced at each of the distances and times noted above would result in a low potential for temporary after-image. Based on FAA Interim Policy for FAA Review of Solar Energy System Projects on Federally Obligated Airports, low potential for temporary after-image is an acceptable level of impact for solar facilities located on airport property. In addition, due to the limited size of the solar panel in comparison to a large solar field, the length of time of glare would be minimal.

Noise: This specific site is located within an area outside the 55 CNEL contour from aircraft. Therefore, no special measures to mitigate aircraft-generated noise are required.

PART 77: Looking at this site and building specifically, the elevation at the southerly end of the runway at Jacqueline Cochran Regional Airport is -137.5 feet above mean sea level (AMSL). At a distance of approximately 4,600 feet from the runway, any building with an elevation at top of roof exceeding -91.0 feet AMSL would require FAA notice and review through the Form 7460-1 process.

The proposed building has a finished floor elevation of -146.67 feet AMSL. At this pad elevation, the proposed building height of 27 feet would result in a maximum site elevation of -119.67 feet AMSL. This would allow up to 28 feet for roof-mounted apparatus before the notification surface is breached. Therefore, review pursuant to the Federal Aviation Administration Obstruction Evaluation Service Form 7460-1 process is not required for the building.

Open Area: Compatibility Zone D requires that 10% of area within major projects (10 acres or larger) be set aside as open land that could potentially serve as emergency landing areas. As detailed

in previous reviews for the larger project, the proposed track areas would provide the most appropriate open area given it typically provides for wide linear areas free of obstructions. As detailed in ZAP1024TH14, 13.47 acres of open space is provided within Zone D which is approximately 26% of the overall 51.6 acres located within Zone D. On the project site alone, open area requirements would not be applicable since the project site is less than 10 acres in size.

CONDITIONS:

These conditions are applicable to the current proposed Lot 202 building photovoltaic solar panels. All other prior recommended conditions from prior ALUC reviews are still applicable to the larger Thermal Motorsports Park project. Certain prior recommended conditions from prior ALUC reviews are also applicable to this project and new conditions are applicable as well. Each condition is noted as such.

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky, and shall comply with Riverside County Ordinance No. 655, as applicable. Outdoor lighting plans, if any, shall be transmitted to Riverside County Economic Development Agency – Aviation Division personnel and to the Jacqueline Cochran Regional Airport for review and comment. (Failure to comment within thirty days shall be considered to constitute acceptability on the part of the airport manager.) [from prior review]
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations, or any type of strobe light, toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, livestock operations, production of cereal grains, sunflower, and row crops, artificial marshes, wastewater management facilities, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, incinerators, and landfills.)

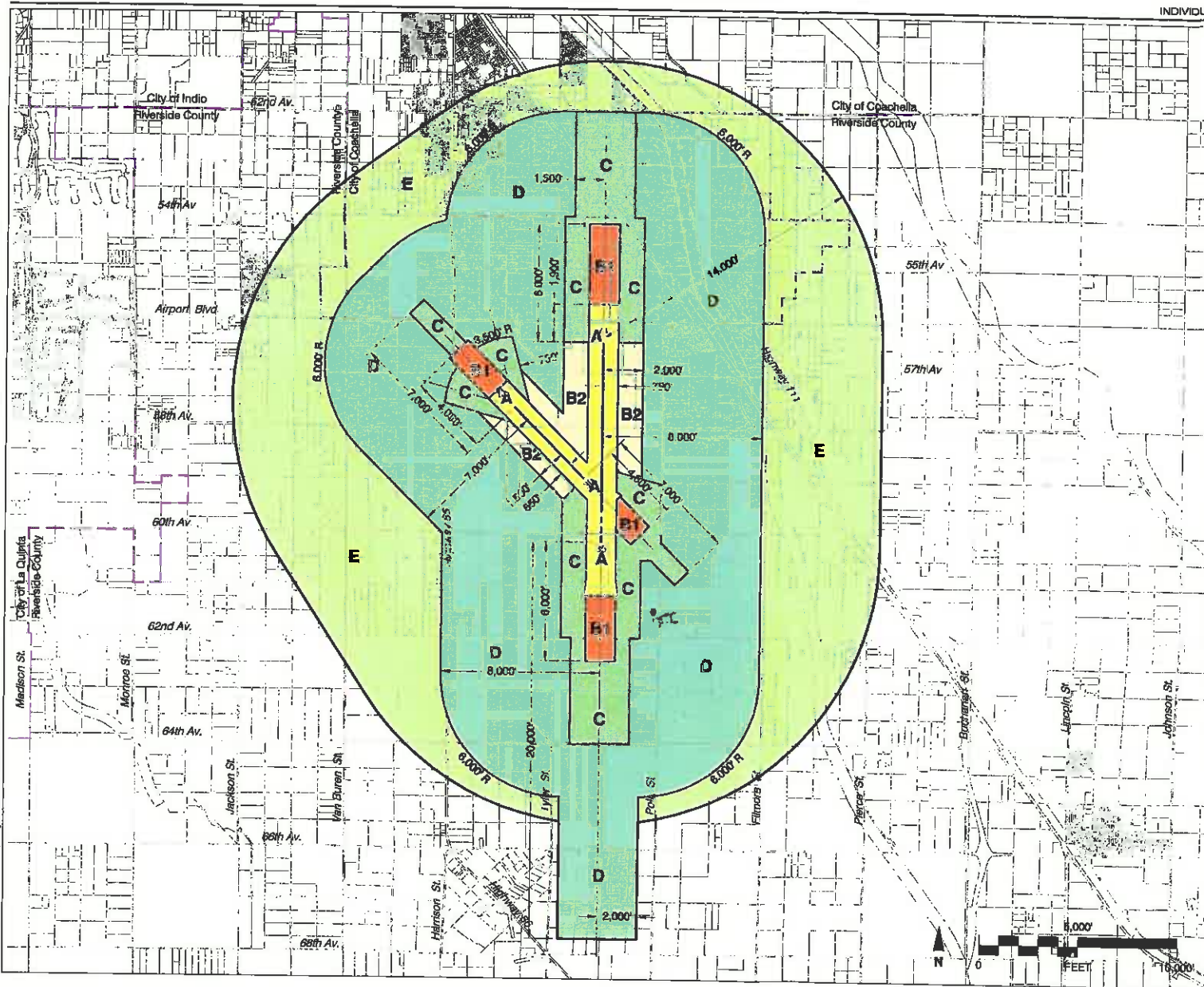
- (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.

[from prior review]

- 3. In the event that any incidence of glint, glare, or flash affecting the safety of air navigation occurs as a result of project operation, upon notification to the airport operator of an incidence, the airport operator shall notify the property owner in writing. Within 30 days of written notice, the property owner shall be required to promptly take all measures necessary to eliminate such glint, glare, or flash. An "incidence" includes any situation that results in an accident, incident, "near-miss," or specific safety complaint regarding an in-flight experience to the airport operator or to federal, state, or county authorities responsible for the safety of air navigation. The property owner shall work with the airport operator to prevent recurrence of the incidence. Suggested measures may include, but are not limited to, seasonally covering the panels at the time of year and/or day when incidences of glare occur to diminish or eliminate the source of the glint, glare, or flash. For each such incidence made known to the property owner, the necessary remediation shall only be considered to have been fulfilled when the airport operator states in writing that the situation has been remediated to the airport operator's satisfaction.
[condition added]








NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Legend

Compatibility Zones

-  Airport Influence Area Boundary
-  Zone A
-  Zone B1
-  Zone B2
-  Zone C
-  Zone D
-  Zone E

Boundary Lines

-  Airport Property Line - Existing
-  Airport Property Line - Planned
-  City Limits

Note

Except for southern extension, Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

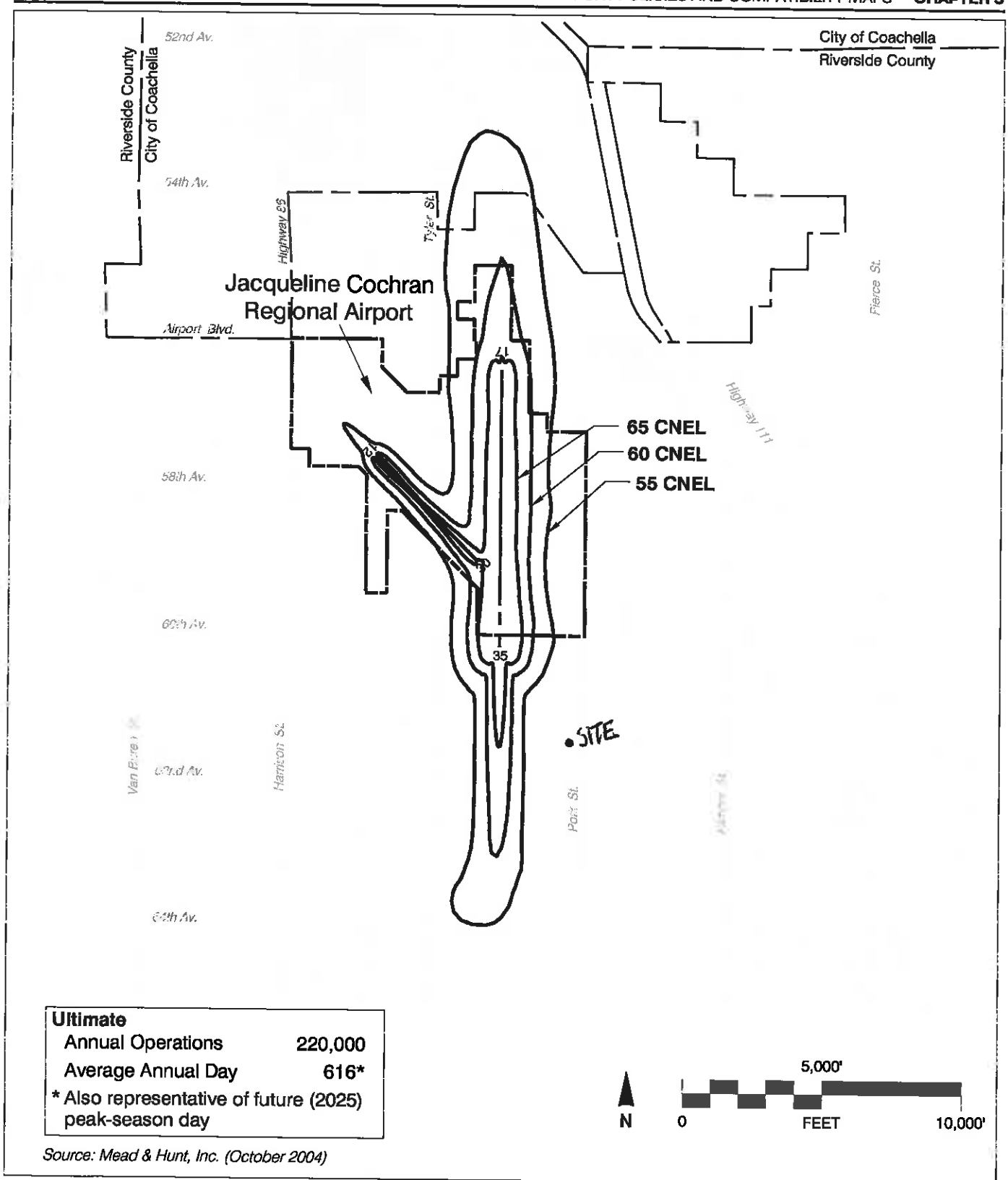
See Chapter 2, Table 2A for compatibility criteria associated with this map.

Riverside County
Airport Land Use Commission
Riverside County
Airport Land Use Compatibility Plan
Policy Document
(Adopted June 2005)

Map JC-1

Compatibility Map
Jacqueline Cochran Regional Airport





Map JC-3

Noise Compatibility Contours

Jacqueline Cochran Regional Airport

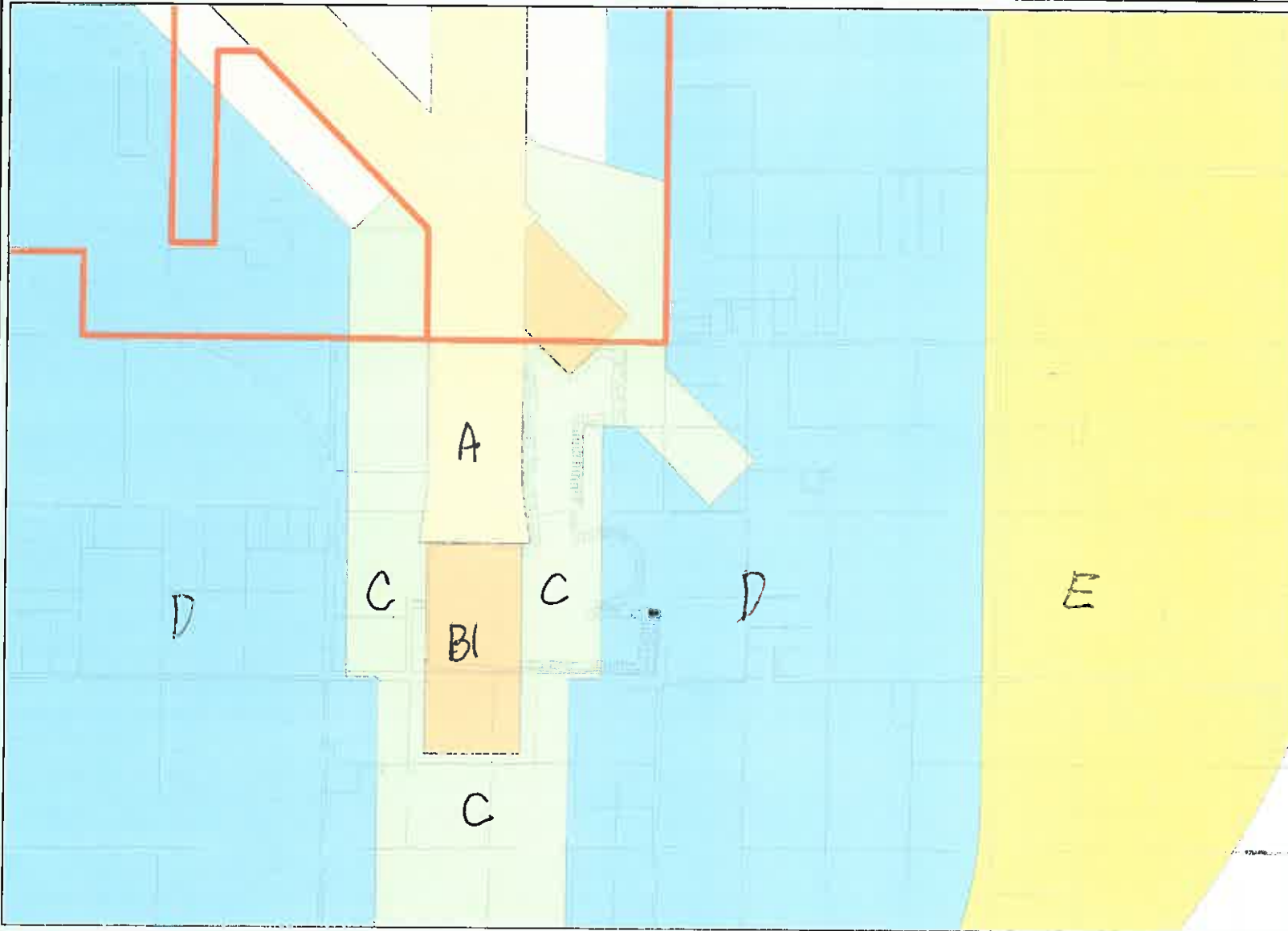
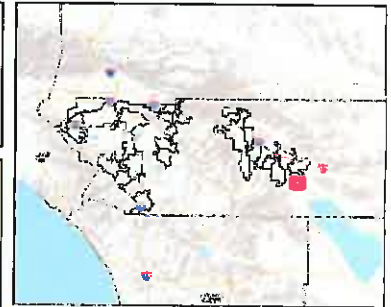
PROJECT LOCATION



RUSSAK (REPRESENTATIVE NICK FULLERTON, FULLERTON ARCHITECTS)

NFA

My Map



Legend

- Airports
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT
- C2-HIGHT-EXC1



0 2,909 5,818 Feet



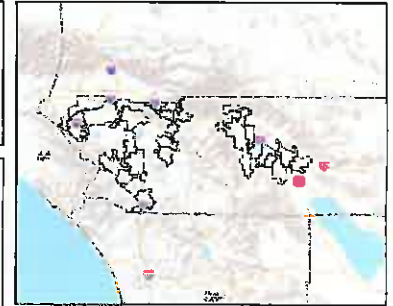
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REPORT PRINTED ON... 8/25/2015 12:14:26 PM

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Notes

My Map



Legend

- RCLIS Parcels
- Airports
- City Boundaries
- Cities
- roadsanno
- highways
 - HWY
 - INTERCHANGE
 - INTERSTATE
 - OFFRAMP
 - ONRAMP
 - USHWY
- counties
- cities
- hydrographylines
- waterbodies
 - Lakes
 - Rivers



0 1,454 2,909 Feet



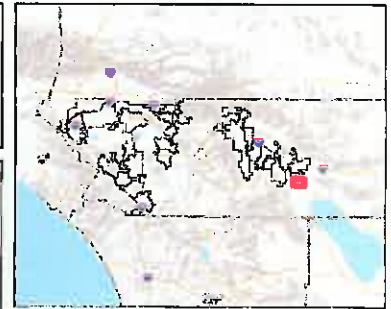
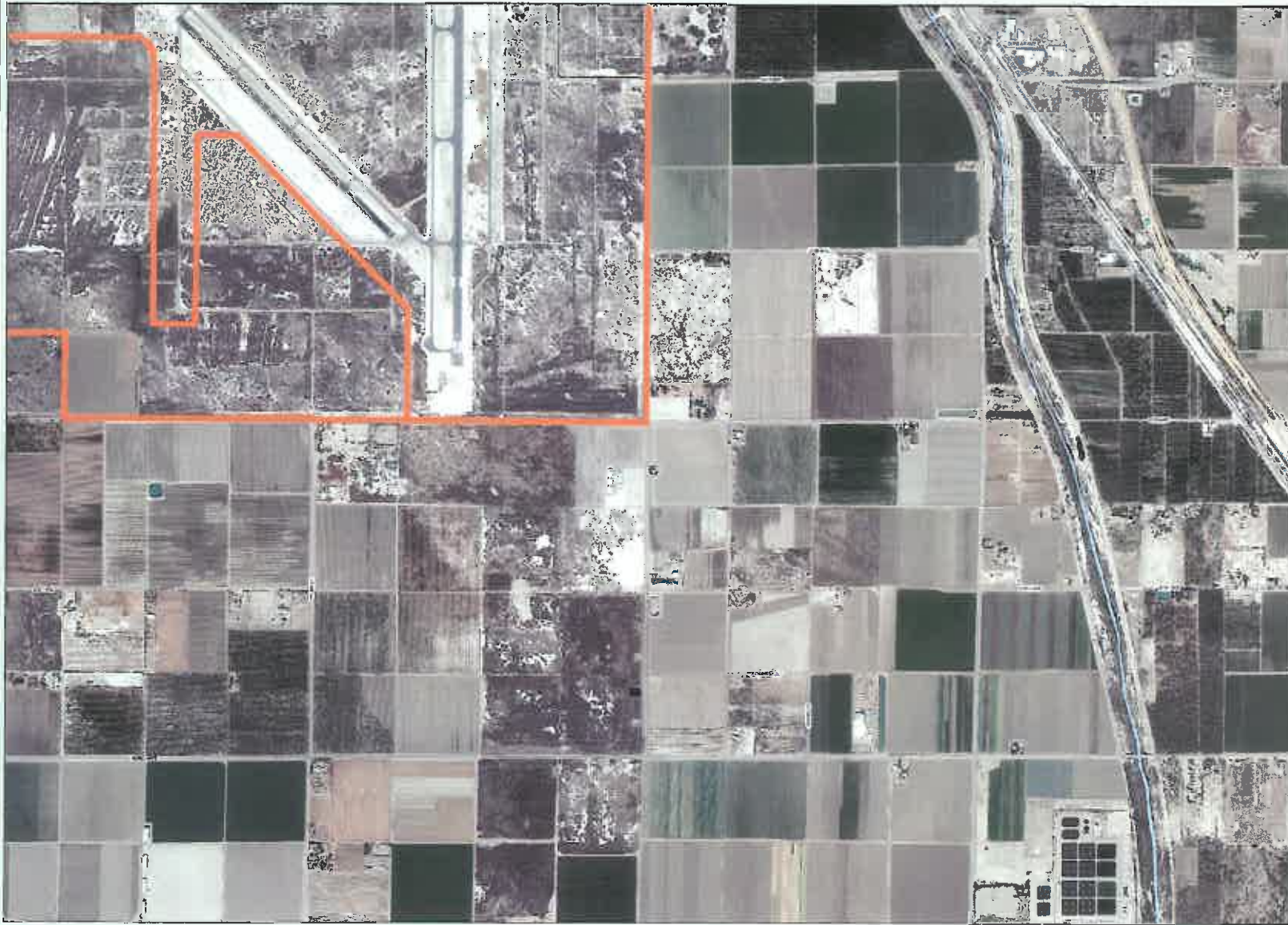
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Notes

My Map



Legend

- Airports
- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 2,909 5,818 Feet



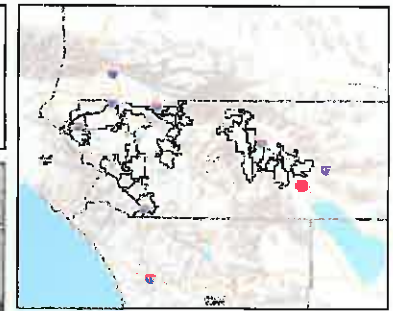
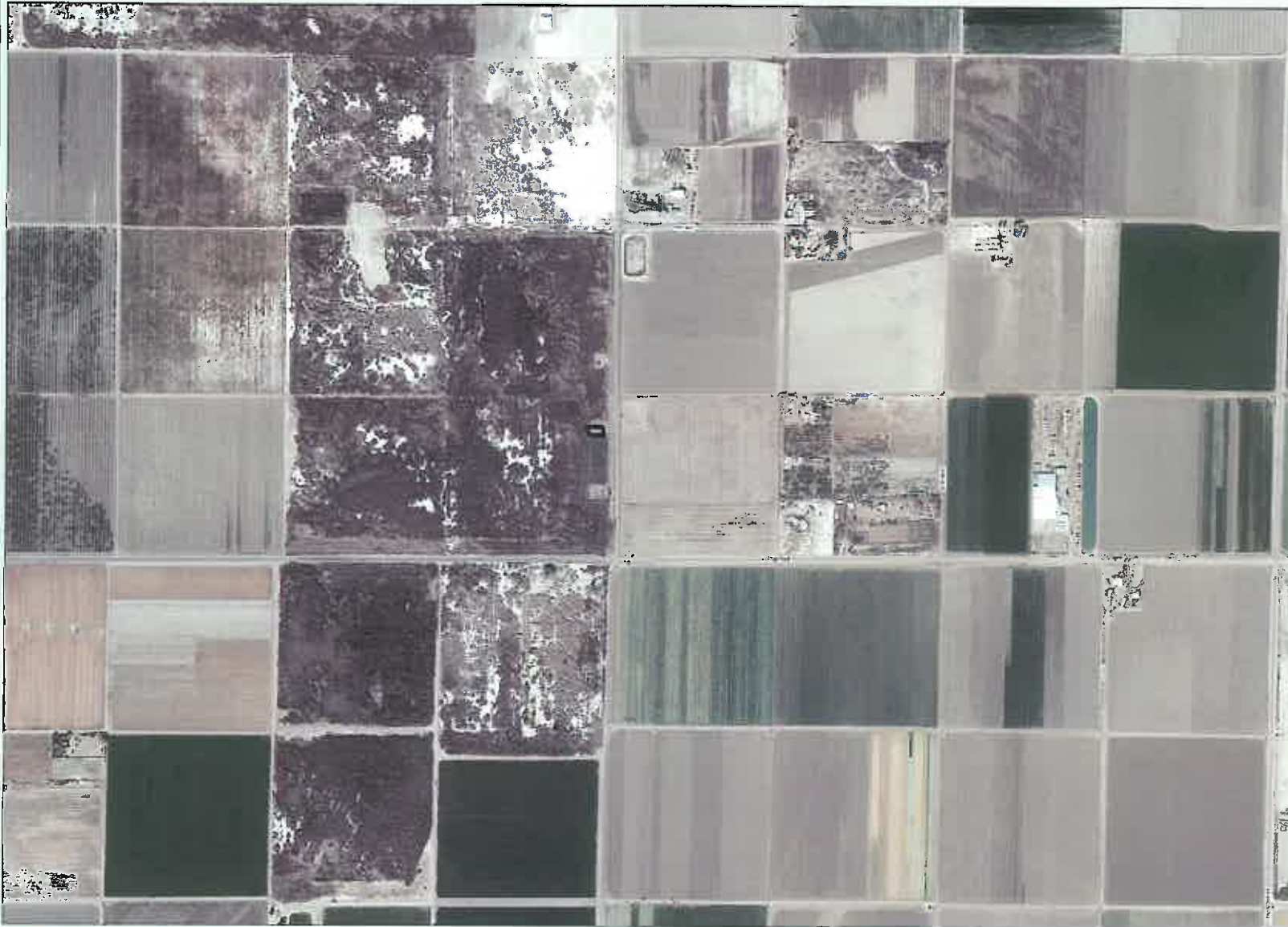
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Notes

My Map



Legend

-  RCLIS Parcels
-  Airports
-  City Boundaries
- Cities
- roadsanno
- highways
 -  HWY
 -  INTERCHANGE
 -  INTERSTATE
 -  OFFRAMP
 -  ONRAMP
 -  USHWY
-  counties
-  cities
- hydrographylines
- waterbodies
 -  Lakes
 -  Rivers



0 1,454 2,909 Feet



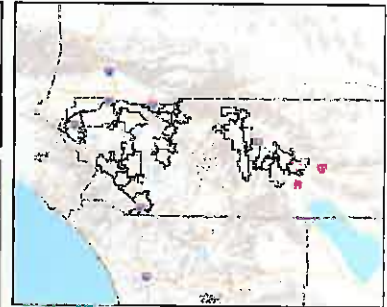
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Notes

My Map



Legend

-  RCLIS Parcels
-  Airports
-  City Boundaries
- Cities
- roadsanno
- highways
-  HWY
-  INTERCHANGE
-  INTERSTATE
-  OFFRAMP
-  ONRAMP
-  USHWY
-  counties
-  cities
- hydrographylines
- waterbodies
-  Lakes
-  Rivers



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0 727 1,454 Feet

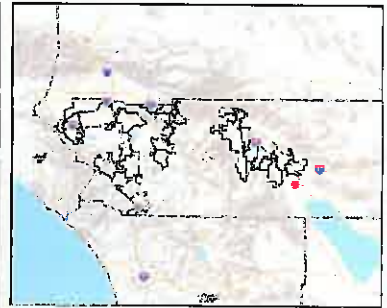


REPORT PRINTED ON... 8/25/2015 12:21:50 PM














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Notes

My Map



Legend

-  RCLIS Parcels
-  Airports
-  City Boundaries
- Cities
- roadsanno
- highways
 -  HWY
 -  INTERCHANGE
 -  INTERSTATE
 -  OFFRAMP
 -  ONRAMP
 -  USHWY
-  counties
-  cities
- hydrographylines
- waterbodies
 -  Lakes
 -  Rivers



0 182 364 Feet

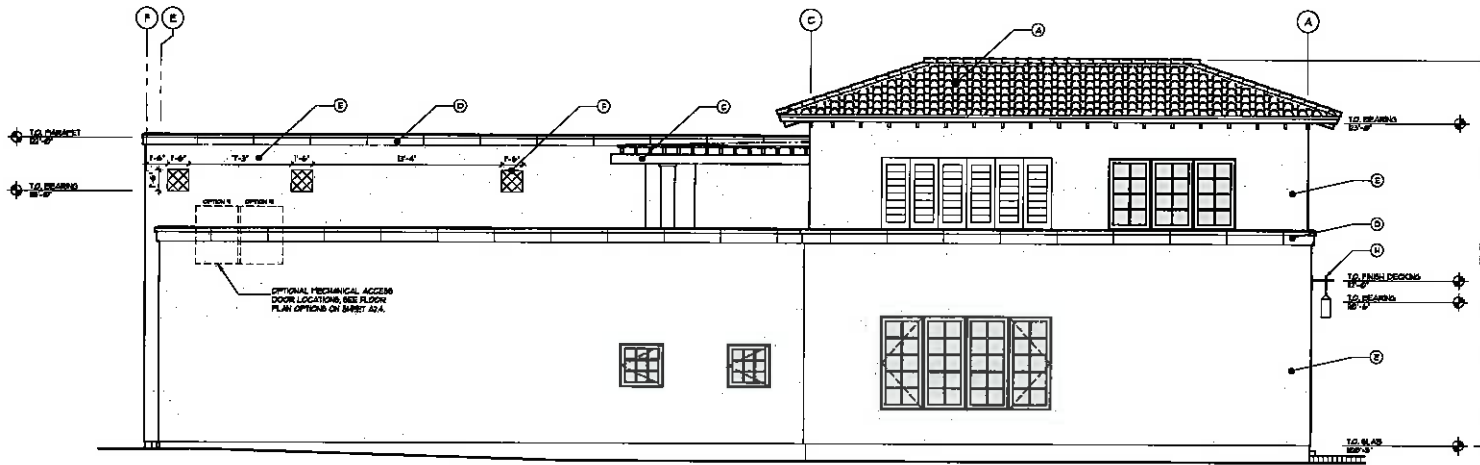


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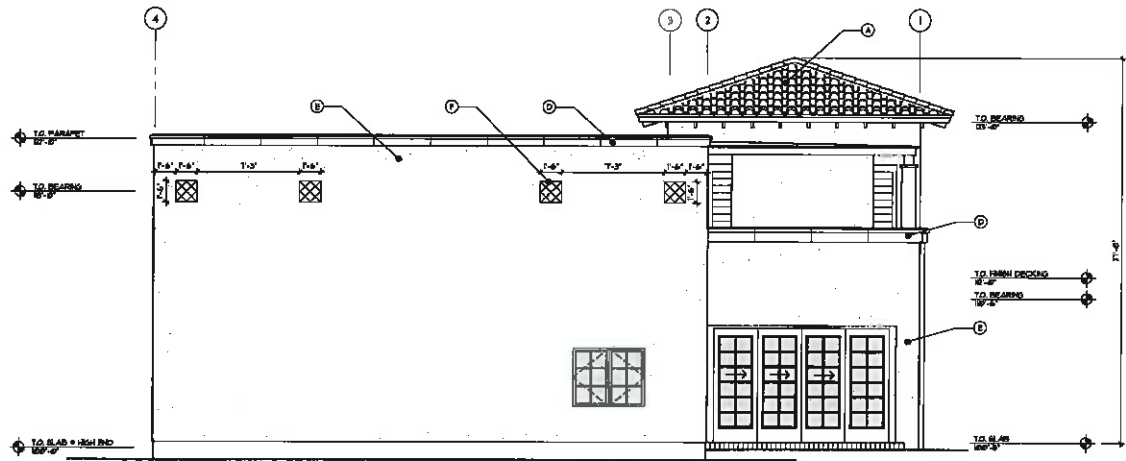
REPORT PRINTED ON... 8/25/2015 12:23:03 PM

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Notes



1 NORTH ELEVATION
SCALE: 1/4" = 1'-0"



2 EAST ELEVATION
SCALE: 1/4" = 1'-0"

EXTERIOR FINISH SCHEDULE	
(A)	RED-LAND, TWO-PIECE CLAY, FORM COLOR BLEND - COLOR SANDCAST PEDIAH PORTLAND, SEA ECOAT, RANDOM BIT HORIZONTAL AS WELL AS VERTICAL
(B)	NS-295 RAFTER TAIL & 34" CG
(C)	RA BEAM (SEE STEEL) STAIN FINISH
(D)	PRE-CAST CONCRETE CAP
(E)	PAINTED EXTERIOR PLASTER IS HARD TROBEL FINISH OVER UCCO HAIRED WALL
(F)	CERAMIC TILE INSET
(G)	OVERHEAD SECTIONAL GARAGE DOOR
(H)	LIGHT FIXTURE

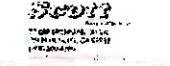
- NOTES
1. ALL FINISHING MATERIAL TO BE GRABBERED COPPER NOT GRABED UNLESS NOTED OTHERWISE
 2. ALL ROOF BRACKS AND DOOR OPENINGS TO BE BULLDOZE PLASTER UNCL.
 3. ALL FASCIA AND FINIC TRIM MATERIALS TO BE ROUGH TEXTURE, PRL STAIN

NOTE:
ALL EXTERIOR LIGHT FIXTURES SHALL COMPLY WITH RIVERSIDE COUNTY ORDINANCE NO. 909, ZONE B REQUIREMENTS, HANGERS AND UPSTAIRS, NUTLY BARELLED, CONTRACTOR TO OBTAIN MANUFACTURER CUT SHEET FOR REVIEW PRIOR TO ORDERING.



mfa
Michael Baker International, Inc.
150 West 17th Street, Suite 1700
Palo Alto, CA 94301
Tel: 650-851-2000 Fax: 650-851-2000
www.michaelbaker.com

Michael Baker INTERNATIONAL
7400 COLONY CIRCLE, SUITE 201
PALO ALTO, CA 94303
REGISTRATION NO. 200-00-7088 3 700-0-0000



No.	Date	Revision

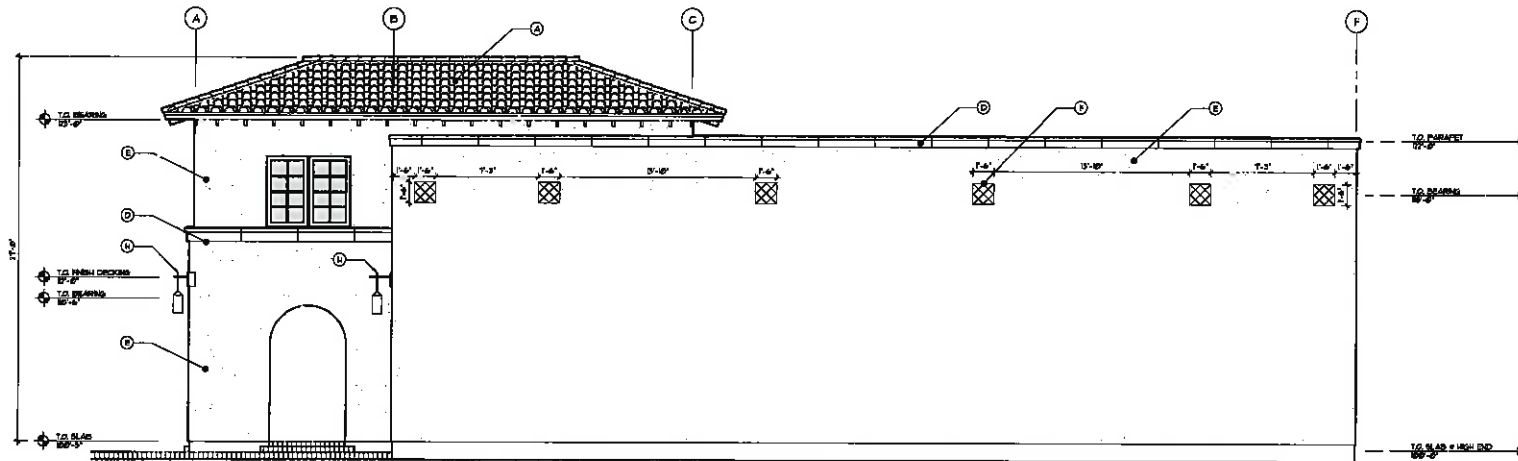
Project Number: 0-2405
NEW TENDER PRIVATE GARAGE FOR ALLEN RUSBAK
LOT 502 - THE THERMAL CLUB
THERMAL, CA

ISSUED FOR PERMIT

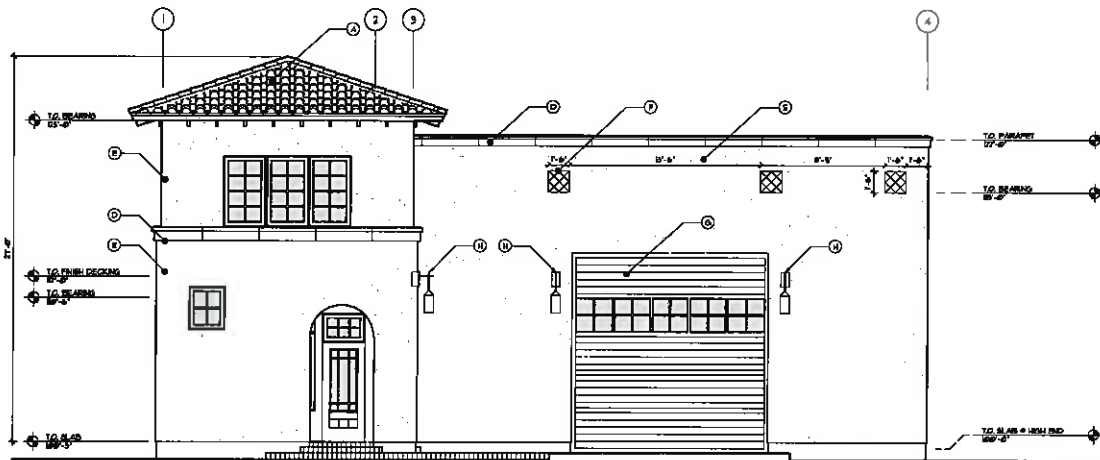
ALL DRAWINGS ON THESE SHEETS SHALL BE AT THE CLIENT'S RISK AND UNDER NO CIRCUMSTANCES SHALL BE REPRODUCED OR COPIED WITHOUT THE WRITTEN PERMISSION OF THE ENGINEER.

Drawn: PJA, DA, AR, RB
Checked: DG
Date: June 18, 2010

Sheet Title:
EXTERIOR ELEVATIONS
Sheet Number:
A3.1



1 SOUTH ELEVATION
SCALE: 1/4" = 1'-0"



2 WEST ELEVATION
SCALE: 1/4" = 1'-0"

EXTERIOR FINISH SCHEDULE

- (A) REDLAND, TWO-PICE CLAY, FOUR COLOR BLEND - COLOR BANDS/SH HEDSH HORIZONTAL FOR BOOH, RANDOM SET HORIZONTAL, AS WELL AS VERTICAL.
- (B) NR. 200 RAFTER TAILS - 24" OC.
- (C) NR. 200 (SEE STRU.) STAIN FINISH
- (D) PRE-CAST CONCRETE CAP
- (E) PAINTED EXTERIOR PLASTER UP HAND TRIM/EL. FINISH, OVER WOOD FRAMED WALL.
- (F) CERAMIC TILE INSET
- (G) OVERHEAD SECTIONAL GARAGE DOOR
- (H) LIGHT FIXTURE

- NOTES:
1. ALL PLASTER MATERIAL TO BE WEATHERED COPPER AND GREEN (UNLESS NOTED OTHERWISE)
 2. ALL WOOD UNDOOR AND DOOR OPENINGS TO BE BULLDOG PLASTER UND.
 3. ALL FACIA AND HING. TRIM MATERIALS TO BE BROWN TEXTURE, NR. 874N.

NOTE:
ALL EXTERIOR LIGHT FIXTURES SHALL COMPLY WITH
SUNSHINE COUNTY ORDINANCE NO. 2008, 2008 B,
REQUIREMENTS, MANUFACTURE APPROVED, FULLY BUILT
CONTRACTORS TO VERIFY MANUFACTURER OFF SHEET FOR
REVIEW PRIOR TO ORDERING.



nfa
NATIONAL FIRE ALARM
NOTIFICATION ASSOCIATION
P.O. Box 2718, Morgan Hill, CA 95037
Ph: 408-931-9999 F: 408-931-3366
C-0004-1000-00000000

Michael Baker
INTERNATIONAL
7400 Country Club Drive, Suite 100
Palo Alto, CA 94302
www.michaelbaker.com
Tel: 650-949-0000 Fax: 650-949-0002

HSA
DESIGN GROUP

ECLIPSE
ENGINEERING
4000 UNIVERSITY AVENUE
DUBLIN, CA 94568
P. 925.842.0000

Scott
SCOTT & ASSOCIATES
11000 BAYVIEW BLVD. SUITE 100
SAN DIEGO, CA 92126
TEL: 619-441-1200



No.	Date	Revision

Project Number: B-001
NEW MEMBER PRIVATE GARAGE FOR
ALLEN
RUBBAK
LOT 1902 - THE THERMAL CLUB
THERMAL, CA

ISSUED FOR PERMIT
All information on sheets addressed
hereon is for informational purposes only.
Drawn: P. D. A. R. S.
Checked: DS
Date: Jun 28, 2018

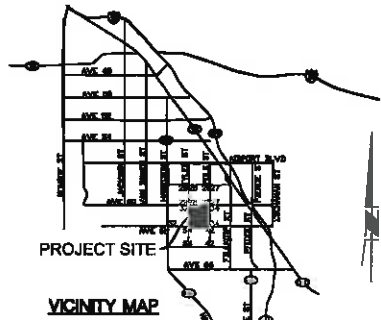
Sheet Title
EXTERIOR
ELEVATIONS
Sheet Number
A32

IN THE UNINCORPORATED TERRITORY OF THERMAL,
COUNTY OF RIVERSIDE, STATE OF CALIFORNIA

Precise Grading Plan

THE THERMAL CLUB PARCEL 202

PARCEL MAP NO. 36293-1 PMB 234/24-50



LEGEND

FINISHED SURFACE.....FS
GRADE BREAK.....GB
HIGH POINT.....HP
TOP OF CURB.....TC
FLOWLINE.....FL
LOW POINT.....LP
PROPERTY LINE.....P
RIGHT-OF-WAY.....R/W
CLUB FACE.....CF
EDGE OF PAVEMENT.....EP
FINISHED GRADE.....FG
EXISTING ELEVATION.....(100.50)
FINISHED ELEVATION.....+101.20
PROPERTY LINE.....E
CONTROLLING.....C
RIGHT-OF-WAY (R/W).....R/W
EXIST. WATER LINE.....W
EXIST. SEWER LINE.....S
EXIST. GAS LINE.....G
EASEMENT LINE.....E
WATER METER.....M
ELECTRICAL WALL.....E

LEGAL DESCRIPTION
PARCEL 202 OF PARCEL MAP 36293-1, ON
FT. 101 BOOK 234, OF PARCEL MAPS, AT
PAGES 24 THROUGH 50, INCLUSIVE,
RECORDS OF RIVERSIDE COUNTY, STATE
OF CALIFORNIA.

APN
756-220-026

PROJECT ADDRESS
PARCEL 202, 61-776 FULLERTON DRIVE
THERMAL, CA 92274

OWNER
NAME: JTM LAND COMPANY, LLC
CONTACT: TIM ROEFS
EMAIL: TIMROEFS@ENERGY.COM
ADDRESS: 1883 WEST 190TH STREET
TORRANCE, CA 90504
TEL. No.: (310) 538-8000

CONTRACTOR
NAME: PINNACLE CUSTOM HOMES
CONTACT: DARREN BRADBURY
ADDRESS: 151 KALJUS DRIVE, STE. B-180
COSTA MESA, CA 92626
TEL. No.: (714) 327-0225

UTILITIES

COACHELLA VALLEY WATER DISTRICT	(760) 398-2651
IMPERIAL IRRIGATION DISTRICT	(800) 303-7758
THE GAS COMPANY	(760) 323-1851
VERIZON	1-800-483-3000
TIME WARNER	(760) 340-1312 EXT.281
UNDERGROUND SERVICE ALERT	1-800-227-2800

**NOTE TO CONTRACTOR
EXISTING UTILITIES**

THE LOCATIONS OF UNDERGROUND UTILITIES SHOWN ON THE PLANS WERE OBTAINED FROM A REVIEW OF AVAILABLE RECORD DATA WHILE THE CURB WAS TAKEN IN PREPARATION. RES CONSULTANTS AND GANNETT DOES NOT GUARANTEE THE ACCURACY NOR THE COMPLETENESS OF THE INFORMATION. THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE MANNER ONLY. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATIONS OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK.

THE CONTRACTOR AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES, WHICH RESULT FROM HIS FAILURE TO LOCATE, EXCAVATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES. THE CONTRACTOR SHALL POTENTIALLY PROPOSE CROSSINGS OF EXISTING UTILITIES PRIOR TO CONSTRUCTION AND SHALL VERIFY UTILITIES OF UNKNOWN STRUCTURES ARE FOUND AT UNEXPECTED ELEVATIONS OR LOCATIONS. THE OWNER AND ENGINEER IS TO BE NOTIFIED OF SUCH CONDITION AT ONCE. THE OWNER AND ENGINEER WILL MAKE REQUIRED DESIGN CHANGES AND THE CONTRACTOR AGREES TO COMPLETE ALL WORK INCLUDING PERMITS REQUIRED IN AN EXPEDITIOUS MANNER. THE CONTRACTOR FURTHER AGREES TO PURSUE QUIETLY THE COMPLETION OF SAID WORK. ANY DISCREPANCY BETWEEN THE PLANS AND FIELD SHALL BE BROUGHT TO THE ATTENTION OF THE OWNER AND ENGINEER.

THE CONTRACTOR AGREES THAT HE SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT INCLUDING SAFETY OF ALL PERSONS AND PROPERTY. THAT THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND SHALL NOT BE LIMITED TO NORMAL WORKING HOURS; AND THAT THE CONTRACTOR SHALL DEFEND, INDEMNIFY AND HOLD THE OWNER AND ENGINEER HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPT FOR LIABILITY ARISING FROM THE NEGLIGENCE OF THE OWNER OR THE ENGINEER.



BASE OF BEARINGS
DESCRIPTION:
BEARINGS FOR THIS PLAN ARE BASED ON THE EAST LINE OF THE SOUTHWEST QUARTER OF SECTION 33, TOWNSHIP 8 SOUTH, RANGE 9 EAST, BEING NORTH 09°00'07" EAST, AS SHOWN IN PARCEL MAP 28293-1, ON FILE IN BOOK 234/PAGES 24 THROUGH 50, INCLUSIVE, RECORDS OF RIVERSIDE COUNTY, CALIFORNIA.

BENCHMARK
DESCRIPTION:
CALTRANS BM NO. RY-14.8 L. 8-1/2" BRASS DISK IN CONCRETE AT THE S.E. CORNER OF HWY 60 AND AVE. 61, STAMPED CALIFORNIA DEPT. OF TRANSPORTATION 1888 R-14.8 L. 0.7' BELOW GROUND AND 3' S.E. OF PP# 8507500
ELEV. = 375.33
LOCAL DATUM = -500' 1872 ADJUSTMENT

BY	MARK	DESCRIPTION	DATE
DESIGNER	REVISIONS	CITY	

Michael Baker INTERNATIONAL
2400 Country Club Drive, Suite 201
Palo Alto, CA 94303
www.mbakergroup.com
760-948-7448 1-760-648-6268



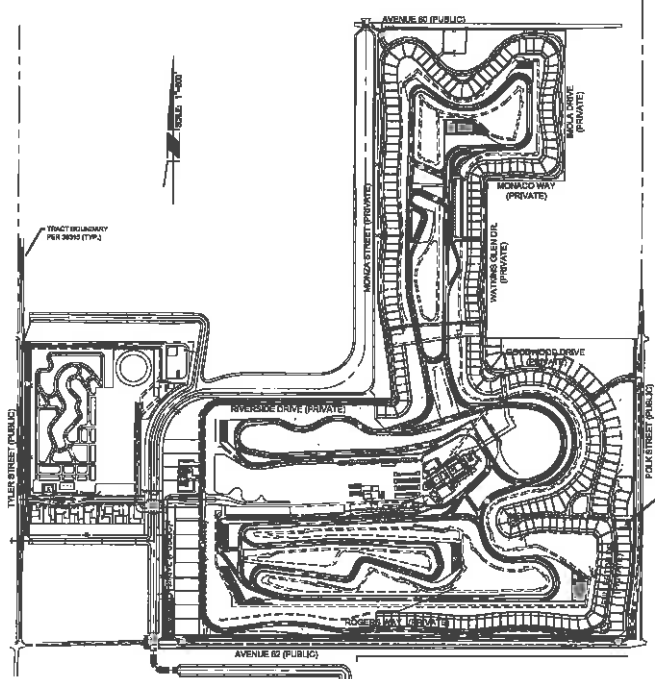
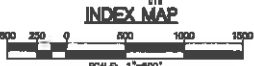
CONSTRUCTION NOTES AND QUANTITY ESTIMATE

NO.	DESCRIPTION	UNIT	QTY
1	CONSTRUCT DRAIN OUTLET PER DETAIL HEREIN.	EA	2
2	INSTALL 4" SCHEDULED 40 PVC DRAIN PIPE.	LF	224
3	INSTALL NO. 800-4" REIN. DRAIN GATE & FITTING PER LANDSCAPE ARCHITECT PLAN.	EA	4
4	CONTRACT HARDWARE PER LANDSCAPE ARCHITECT PLAN.	PER PLAN	
5	SMOOTH 2" CONCRETE RUBBER AND JOIN TO EXISTING.	LF	20

NOTE
QUANTITIES SHOWN ARE ESTIMATES ONLY. THE CONTRACTOR IS RESPONSIBLE FOR PREPARING HIS/HER OWN ESTIMATES FOR BID PURPOSES.

INDEX OF SHEETS

SHEET	DESCRIPTION
1	TITLE SHEET * VICINITY MAP * GEN. NOTES
2	PRECISE GRADING PLAN, DETAILS, TYPICAL SECTIONS



GRADING NOTES (2013 CBC)

- ALL GRADING SHALL CONFORM TO THE 2013 CALIFORNIA BUILDING CODE CHAPTERS 17, 18 & APPENDIX CHAPTER - J AS AMENDED BY ORD. 457.
- ALL PROPERTY CORNERS SHALL BE CLEARLY DELINEATED IN THE FIELD PRIOR TO COMMENCEMENT OF ANY CONSTRUCTION/GRADING.
- ALL WORK UNDER THIS GRADING PERMIT SHALL BE LIMITED TO WORK WITHIN THE PROPERTY LINES. ALL WORK WITHIN THE ROAD RIGHT-OF-WAY WILL REQUIRE SEPARATE PLANS AND A SEPARATE REVIEW/APPROVAL (PERMIT) FROM THE TRANSPORTATION DEPARTMENT.
- GRADING SHALL BE DONE UNDER THE SUPERVISION OF A SOILS ENGINEER IN CONFORMANCE WITH RECOMMENDATIONS OF THE PRELIMINARY GEOTECHNICAL INVESTIGATION BY SLADDER ENGINEERING DATED 9/4/12 AND COMPACTION REPORT DATED 12/9/13.
- COMPACTED FILL TO SUPPORT ANY STRUCTURES SHALL COMPLY WITH SECTION 1803.5. PRODUCTS WITHOUT PRELIMINARY SOILS REPORT SHALL HAVE DETAILED SPECIFICATIONS SATISFYING THE REQUIREMENTS IN SECTION 1803.5 PREPARED BY THE EOR.
- THE CONTRACTOR SHALL NOTIFY THE BUILDING AND SAFETY DEPARTMENT AT LEAST 24 HOURS IN ADVANCE TO REQUEST FINISH LOT GRADE AND DRAINAGE INSPECTION. THIS INSPECTION MUST BE APPROVED PRIOR TO BUILDING PERMIT FINAL INSPECTION FOR EACH LOT.
- THE CONTRACTOR SHALL NOTIFY UNDERGROUND SERVICE ALERT, TWO DAYS BEFORE DIGGING AT 1-800-483-4133.
- CUT/FILL
8. MAXIMUM CUT AND FILL SLOPE = 2:1.
- NO FILL SHALL BE PLACED ON EXISTING GROUND UNTIL THE GROUND HAS BEEN CLEARED OF WEEDS, DEBRIS, TOPSOIL AND OTHER DELETERIOUS MATERIAL. FILLS SHOULD BE PLACED IN THE LOTS (8-INCH MAX OR AS RECOMMENDED IN SOILS REPORT), COMPACTED AND TESTED AS GRADING PROGRESSES UNTIL FINAL GRADINGS ARE ATTAINED. ALL FILLS ON SLOPES STEEPER THAN 5 TO 1 (H/V) AND A HEIGHT GREATER THAN 5 FEET SHALL BE KEYED AND BENCHED INTO FIRM NATURAL SOIL FOR FULL SUPPORT. THE BENCH UNDER THE TOE MUST BE 10 FEET WIDE MIN.
- THE SLOPE STABILITY FOR CUT AND FILL SLOPES OVER 30' IN VERTICAL HEIGHT, OR SLOPES STEEPER THAN 2:1 MUST BE VERIFIED WITH A FACTOR OF SAFETY OF AT LEAST 1.5.
- NO ROCK OR SIMILAR IRREDUCIBLE MATERIAL WITH A MAXIMUM DIMENSION GREATER THAN 12 INCHES SHALL BE BURIED OR PLACED IN FILLS CLOSER THAN 10 FEET TO THE FINISHED GRADE, DRAINAGE AND EROSION/ DUST CONTROL.

- DRAINAGE AND EROSION/DUST CONTROL**
- DRAINAGE ACROSS THE PROPERTY LINE SHALL NOT EXCEED THAT WHICH EXISTED PRIOR TO GRADING. EXCESS OR CONCENTRATED DRAINAGE SHALL BE CONTAINED ON SITE OR DIRECTED TO AN APPROVED DRAINAGE FACILITY.
 - PROVIDE A SLOPE INTERCEPTOR DRAIN ALONG THE TOP OF CUT SLOPES WHERE THE DRAINAGE PATH IS GREATER THAN 40 FEET TOWARDS THE CUT SLOPE.
 - PROVIDE 6" WIDE BY 1' HIGH BERM ALONG THE TOP OF ALL FILL SLOPES STEEPER THAN 3:1.
 - THE GROUND IMMEDIATELY ADJACENT TO THE BUILDING FOUNDATION SHALL BE SLOPED AWAY WITH 5/8" MIN FOR A MIN DISTANCE OF 10 HORIZONTAL FEET. SWALES WITHIN 10 FEET FROM BUILDING SHALL HAVE 2% MINIMUM SLOPE.
 - NO OBSTRUCTION OF NATURAL WATER COURSES SHALL BE PERMITTED.
 - DURING ROUGH GRADING OPERATIONS AND PRIOR TO CONSTRUCTION OF PERMANENT DRAINAGE STRUCTURES, TEMPORARY DRAINAGE CONTROL (BEST MANAGEMENT PRACTICES, BMPs) SHALL BE PROVIDED TO PREVENT FLOWING WATER AND DAMAGE TO ADJACENT PROPERTIES.
 - DUST SHALL BE CONTROLLED BY WATERING OR OTHER APPROVED METHODS.
 - ALL EXISTING DRAINAGE COURSES ON THE PROJECT SITE MUST CONTINUE TO FUNCTION. PROTECTIVE MEASURES AND TEMPORARY DRAINAGE PROVISIONS MUST BE USED TO PROTECT ADJACENT PROPERTIES DURING GRADING OPERATIONS.
 - FOR SLOPES 3 TO 1 (H/V) OR STEEPER: ALL SLOPES EQUAL TO OR GREATER THAN 3' IN VERTICAL HEIGHT, ARE REQUIRED TO BE PLANTED WITH GRASS OR ROSSA ICE PLANT (OR EQUAL) GRASSING COVER AT A MINIMUM SPACING OF 12" ON CENTER. SLOPES EXCEEDING 15' IN VERTICAL HEIGHT SHALL BE PLANTED WITH APPROVED SHRUBS NOT TO EXCEED 10' ON CENTER, OR TREES SPACED NOT TO EXCEED 30' ON CENTER OR SHRUBS NOT TO EXCEED 15' ON CENTER. A COMBINATION OF SHRUBS AND TREES NOT TO EXCEED 15' IN ADDITION TO THE GRASS OR GROUND COVER, SLOPES THAT REQUIRE PLANTING SHALL BE PROVIDED WITH AN IN-GROUND IRRIGATION SYSTEM EQUIPPED WITH AN APPROPRIATE BACKFLOW DEVICE PER U.P.C., CHAPTER 10. THE SLOPE PLANTING AND IRRIGATION SYSTEM SHALL BE INSTALLED PRIOR TO PRECISE GRADING FINAL.
 - COMPLETION OF WORK
21. A REGISTERED CIVIL ENGINEER SHALL PREPARE FINAL COMPACTION REPORT/ GRADING REPORT AND IT SHALL BE SUBMITTED FOR REVIEW AND APPROVAL. THE REPORT SHALL ALSO PROVIDE BUILDING FOUNDATION DESIGN PARAMETERS INCLUDING ALLOWABLE SOIL PRESSURES, EXPANSION INDEX AND REMEDIAL MEASURES IF > 20, WATER SOLUBLE SULFATE CONTENT, CORROSION AND REMEDIAL MEASURES IF NECESSARY.
 - EXCEPT FOR NON-TRACT SINGLE RESIDENTIAL LOT GRADING, THE COMPACTION REPORT SHALL INCLUDE THE SPECIAL INSPECTION VERIFICATIONS LISTED IN TABLE 1704.7 OF 2010 CBC.
 - A REGISTERED CIVIL ENGINEER SHALL SUBMIT TO THE BUILDING AND SAFETY DEPARTMENT WRITTEN CERTIFICATION OF COMPLIANCE WITH THIS GRADING PLAN PRIOR TO REQUESTING INSPECTION AND ISSUANCE OF THE BUILDING PERMIT.

NOTES
1. CONFORM TO ANTIWASH, GEOTECHNICAL DATUM OF 1872, SUBTRACT 800 FROM ALL ELEVATIONS SHOWN ON THESE PLANS.
2. EXISTING IRRIGATION DISTRIBUTION AND IRRIGATION DRAIN MAINS TO BE PROTECTED THROUGHOUT THE COURSE OF THE PROJECT

DESIGN BY: _____ DATE: _____
DRAWN BY: _____ DATE: _____
CHECKED BY: _____ DATE: _____
SCALE: (AS SHOWN)
DATE: _____ TIME: 2:15
JOB NO.: 14735

APPROVED BY: _____ DATE: _____
Administrative Engineer

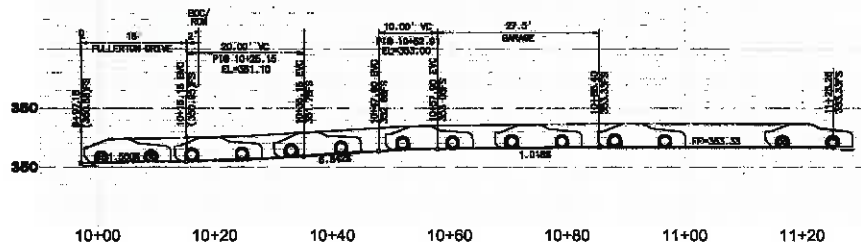
APPROVED AS TO CONFORMANCE WITH APPLICABLE COUNTY STANDARDS AND PRACTICES

IN THE UNINCORPORATED TERRITORY OF THERMAL,
COUNTY OF RIVERSIDE, STATE OF CALIFORNIA

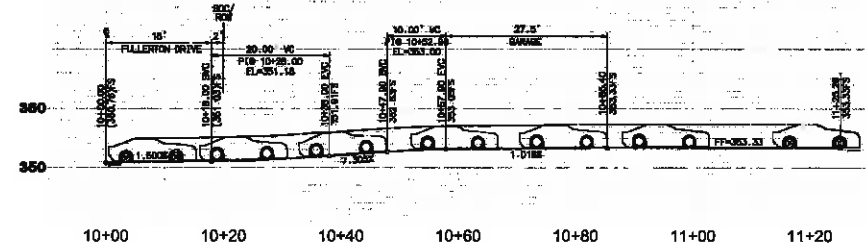
PRECISE GRADING PLAN
THE THERMAL CLUB
PARCEL 202
PARCEL MAP NO. 36293-1 PMB 234/24-50
TITLE SHEET

SHEET 1
OF 2 SHEETS
FILE NO. _____

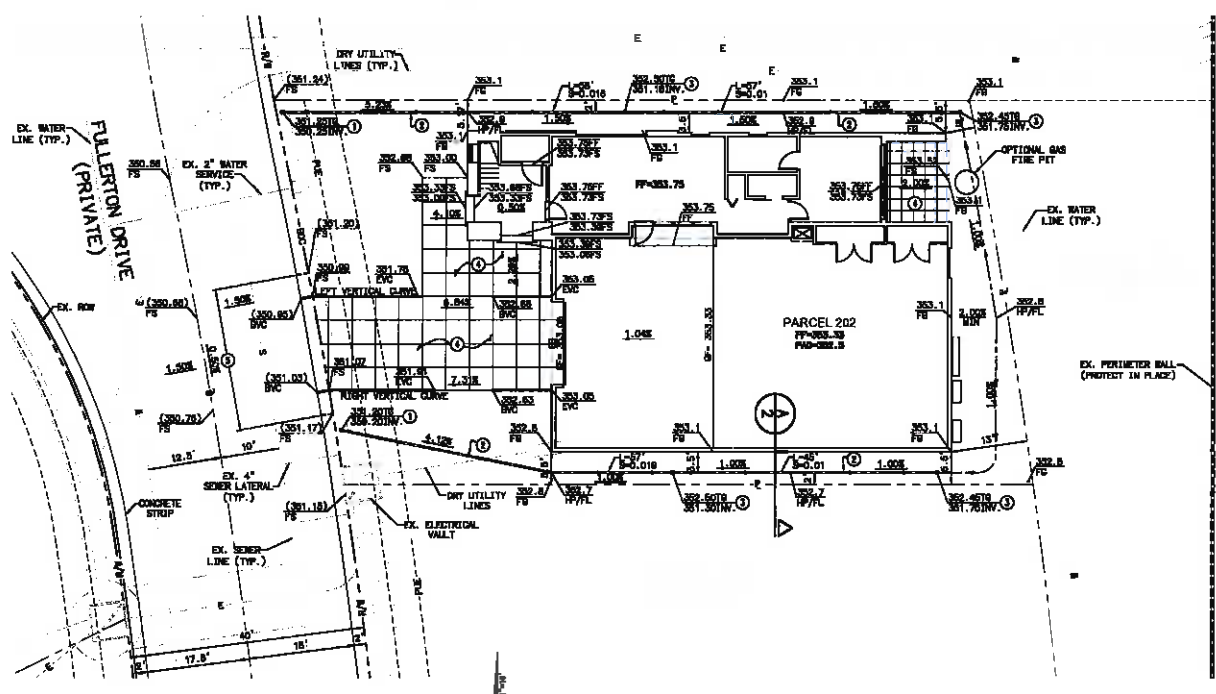
THE THERMAL CLUB



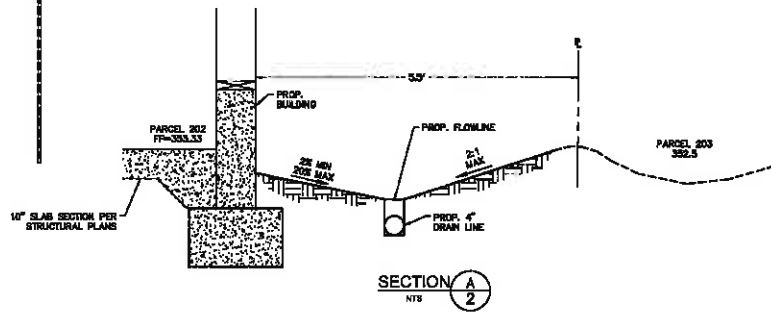
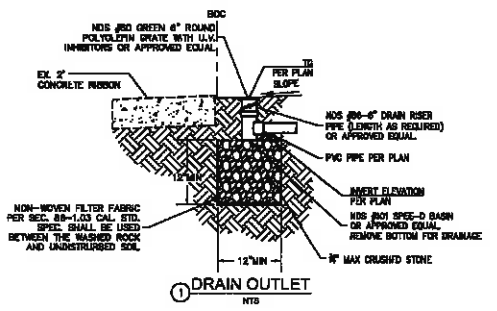
LEFT VERTICAL CURVE
HOR: 1"=10'
VERT: 1"=10'



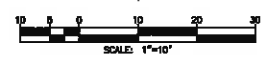
RIGHT VERTICAL CURVE
HOR: 1"=10'
VERT: 1"=10'



- CONSTRUCTION NOTES**
1. CONSTRUCT DRAIN OUTLET PER DETAIL HEREON.
 2. INSTALL 4" SCHEDULED 40 PVC DRAIN PIPE.
 3. INSTALL NOS 800-6" ROUND DRAIN GATE & FITTING PER LANDSCAPE ARCHITECT PLAN.
 4. CONSTRUCT HARDSCAPE PER LANDSCAPE ARCHITECT PLAN.
 5. SAW CUT 2" CONCRETE RIBBON AND JOIN TO EXISTING.



NOTE
WORK CONTAINED WITHIN THESE PLANS SHALL NOT COMMENCE UNTIL AN ENCROACHMENT PERMIT AND/OR A GRADING PERMIT HAS BEEN ISSUED.
THE PRIVATE ENGINEER BEHIND THESE PLANS IS RESPONSIBLE FOR ASSURING THE ACCURACY AND ADEQUACY OF THE DESIGN HEREON. IN THE EVENT OF MISCONDUCTED AND/OR CITY APPROVAL OR DURING CONSTRUCTION, THE PRIVATE ENGINEER SHALL BE RESPONSIBLE FOR DETERMINING AN ACCEPTABLE SOLUTION AND SUBMITTING THE PLANS FOR APPROVAL BY THE CITY.



Michael Baker INTERNATIONAL
7450 Country Club Drive, Suite 500
Palo Alto, CA 94302
www.mbakercorp.com
780-24-7468 / 780-246-6262

DESIGN BY: [Signature]	Reviewed By: [Signature]	Date: _____
CHECKED BY: [Signature]	Approved By: [Signature]	Date: _____
DATE: JUNE 2015	APPROVED AS TO CONFORMANCE WITH APPLICABLE COUNTY STANDARDS AND PRACTICES	

IN THE UNINCORPORATED TERRITORY OF THERMAL, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA

THE THERMAL CLUB
PARCEL 202
PARCEL MAP NO. 38288-1 PMB 234/24-50
PRECISE GRADING PLAN, DETAILS, TYPICAL SECTIONS

SHEET **2** OF 2 SHEETS
FILE NO. _____

DIG ALERT
800.955.7688
1(800) 227-0500
An Alert Site Warning Sign
FOR THE PUBLIC

BY	MARK	DESCRIPTION	APP. DATE	CITY

RIVERSIDE AIRPORT LAND US COMMISSION

BRS 150586

JULY 07, 2015

RUSSAK (REPRESENTATIVE NICK FULLERTON, FULLERTON ARCHITECTS)

NFA

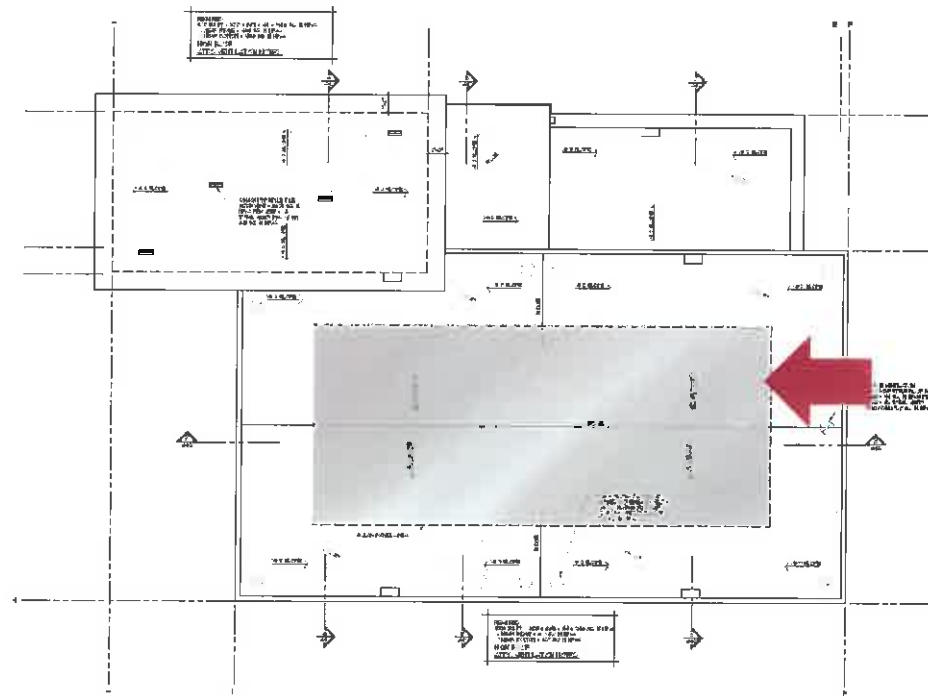
GLARE POTENTIAL FROM 4KW ROOFTOP SOLAR ARRAY

RUSSAK (REPRESENTATIVE NICK FULLERTON, FULLERTON ARCHITECTS)

NFA

PROJECT PROPOSAL

ROOFTOP SOLAR INSTALLATION APPROXIMATELY 4KW



RUSSAK (REPRESENTATIVE NICK FULLERTON, FULLERTON ARCHITECTS)

NFA

PROJECT LOCATION



RUSSAK (REPRESENTATIVE NICK FULLERTON, FULLERTON ARCHITECTS)

NFA

GLARE ANALYSIS

FLIGHT PATH 1 AND 2

09/20/15 Solar Glare Hazard Analysis Flight Path Report


Solar Glare Hazard Analysis Flight Path Report

Generated June 8, 2015, 10:35 a.m.

Flight path: 1

Glare found

Print



http://rusnak.com/.../15/09/20/15

1/4

09/20/15 Solar Glare Hazard Analysis Flight Path Report


Solar Glare Hazard Analysis Flight Path Report

Generated June 8, 2015, 10:35 a.m.

Flight path: 2

Glare found

Print



http://rusnak.com/.../15/09/20/15

1/5

RUSSAK (REPRESENTATIVE NICK FULLERTON, FULLERTON ARCHITECTS)

NFA

GLARE ANALYSIS

Solar Glare Hazard Analysis Tool Report		
Analysis & PV array parameters		
Analysis name	Russak	
PV array axis tracking	none	
Orientation of array (deg)	180.0	
Tilt of solar panels (deg)	27.0	
Rated power (kW)	0.0	
Vary reflectivity	True	
PV surface material	Smooth glass without ARC	
Timezone offset	-8.0	
Subtended angle of sun (mrad)	9.3	
Peak DNI (W/m ²)	1000.0	
Ocular transmission coefficient	0.6	
Pupil diameter (m)	0.002	
Eye focal length (m)	0.017	
Time interval (min)	1	
Consider slope error with material	False	
Slope error (mrad)	10.0	
Flight path parameters		
Direction (deg)	0.72	
Glare slope (deg)	0.0	
Consider pilot visibility from cockpit	True	
Transition to large aircraft		0/1

Solar Glare Hazard Analysis Tool Report		
Analysis & PV array parameters		
Analysis name	Russak	
PV array axis tracking	none	
Orientation of array (deg)	180.0	
Tilt of solar panels (deg)	27.0	
Rated power (kW)	0.0	
Vary reflectivity	True	
PV surface material	Smooth glass without ARC	
Timezone offset	-8.0	
Subtended angle of sun (mrad)	9.3	
Peak DNI (W/m ²)	1000.0	
Ocular transmission coefficient	0.6	
Pupil diameter (m)	0.002	
Eye focal length (m)	0.017	
Time interval (min)	1	
Consider slope error with material	False	
Slope error (mrad)	10.0	
Flight path parameters		
Direction (deg)	0.1416	
Glare slope (deg)	0.0	
Consider pilot visibility from cockpit	True	
Transition to large aircraft		0/1

RUSSAK (REPRESENTATIVE NICK FULLERTON, FULLERTON ARCHITECTS)

NFA

GLARE ANALYSIS

00275 Color-Glare Hazard Analysis Tool Report

Max. downward viewing angle (deg)	30.0
Azimuthal viewing angle (deg)	160.0

00275 Color-Glare Hazard Analysis Tool Report

00275 Color-Glare Hazard Analysis Tool Report

Max. downward viewing angle (deg)	30.0
Azimuthal viewing angle (deg)	160.0

00275 Color-Glare Hazard Analysis Tool Report

RUSSAK (REPRESENTATIVE NICK FULLERTON, FULLERTON ARCHITECTS)

NFA

GLARE ANALYSIS

9/20/18 Solar Glare Hazard Analysis Tool Report

PV array vertices

id	Latitude (deg)	Longitude (deg)	Ground Elevation (ft)	Height of panels above ground (ft)	Total elevation (ft)
1	33.600926053	-116.147126532	-151.0	20.0	-131.0
2	33.600926053	-116.147032455	-151.0	20.0	-131.0
3	33.600777112	-116.147032455	-151.0	20.0	-131.0
4	33.600777112	-116.147126532	-151.0	20.0	-131.0

Flight Path Observation Points

	Latitude (deg)	Longitude (deg)	Ground Elevation (ft)	Eye-level height above ground (ft)	Glare?
Threshold	33.6151156375	-116.153420244	0.0	50.0	No
1/4 mi	33.6115061792	-116.162434537	0.0	118.17	No
1/2 mi	33.607892721	-116.16565943	0.0	166.36	No
3/4 mi	33.6042792328	-116.163554023	0.0	237.55	No
1 mi	33.6006656046	-116.153548616	0.0	326.71	Yes
1 1/4 mi	33.5970523454	-116.153703209	0.0	395.89	Yes
1 1/2 mi	33.5934389881	-116.155757602	0.0	433.07	No
1 3/4 mi	33.5898256429	-116.163512394	0.0	534.26	No
2 mi	33.586212277	-116.165866587	0.0	603.45	No

Glare occurrence plots

All times are in standard time. For Daylight Savings Time add one hour.

<https://sharepoint.officeappsweb/ghgl>

4/6

9/20/18 Solar Glare Hazard Analysis Tool Report

PV array vertices

id	Latitude (deg)	Longitude (deg)	Ground Elevation (ft)	Height of panels above ground (ft)	Total elevation (ft)
1	33.600926053	-116.147126532	-151.0	20.0	-131.0
2	33.600926053	-116.147032455	-151.0	20.0	-131.0
3	33.600777112	-116.147032455	-151.0	20.0	-131.0
4	33.600777112	-116.147126532	-151.0	20.0	-131.0

Flight Path Observation Points

	Latitude (deg)	Longitude (deg)	Ground Elevation (ft)	Eye-level height above ground (ft)	Glare?
Threshold	33.6205947134	-116.153434919	0.0	50.0	No
1/4 mi	33.6169771485	-116.155517453	0.0	118.17	No
1/2 mi	33.6133595819	-116.153200486	0.0	166.36	No
3/4 mi	33.6131420159	-116.155063070	0.0	257.53	No
1 mi	33.6105244487	-116.145963957	0.0	326.71	No
1 1/4 mi	33.6069106621	-116.143649741	0.0	395.89	No
1 1/2 mi	33.6032969155	-116.140782625	0.0	433.07	No
1 3/4 mi	33.6006717489	-116.137011251	0.0	534.26	No
2 mi	33.6005541623	-116.134456565	0.0	603.45	Yes

Glare occurrence plots

All times are in standard time. For Daylight Savings Time add one hour.

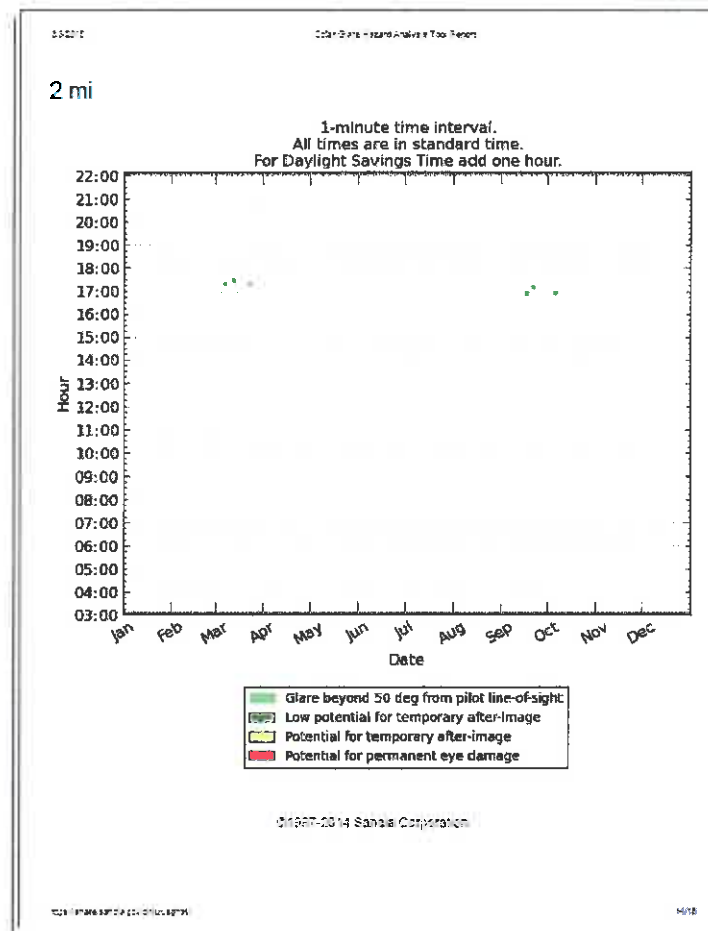
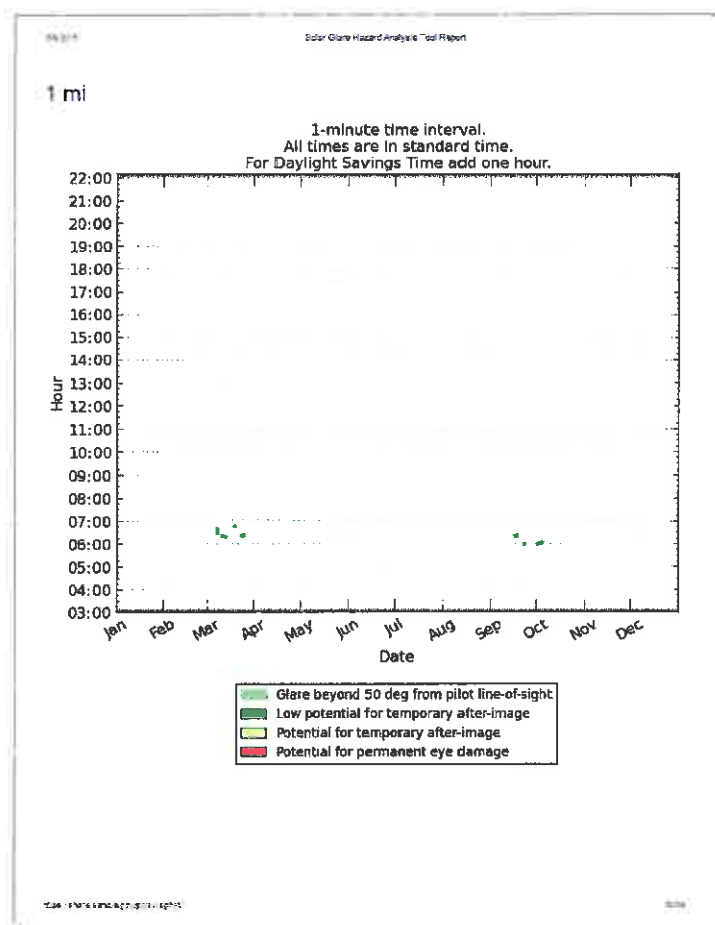
<https://sharepoint.officeappsweb/ghgl>

4/6

RUSSAK (REPRESENTATIVE NICK FULLERTON, FULLERTON ARCHITECTS)

NFA

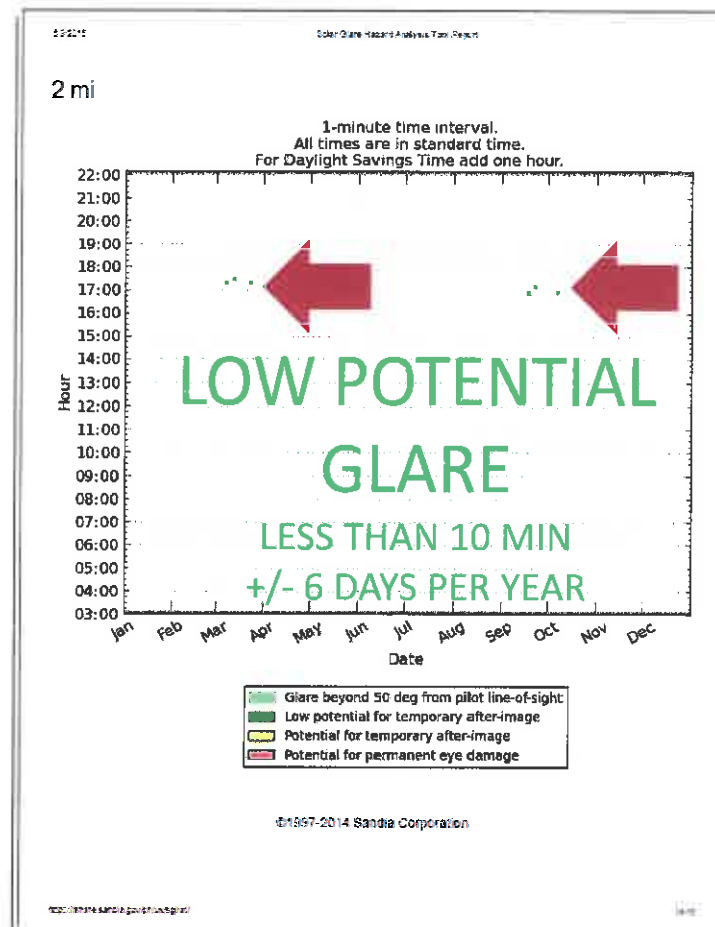
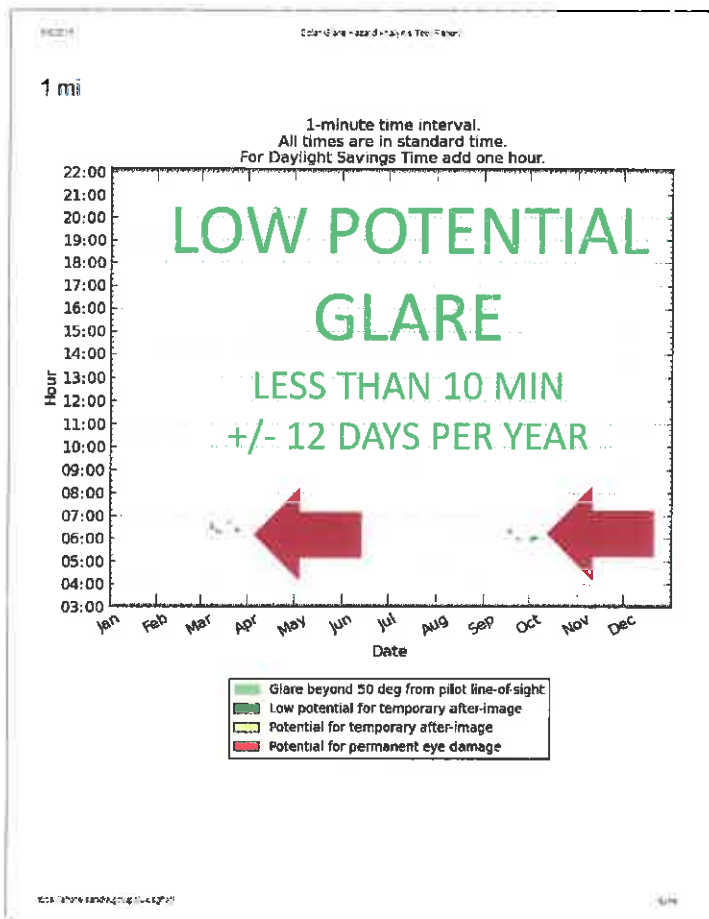
GLARE ANALYSIS



RUSSAK (REPRESENTATIVE NICK FULLERTON, FULLERTON ARCHITECTS)

NFA

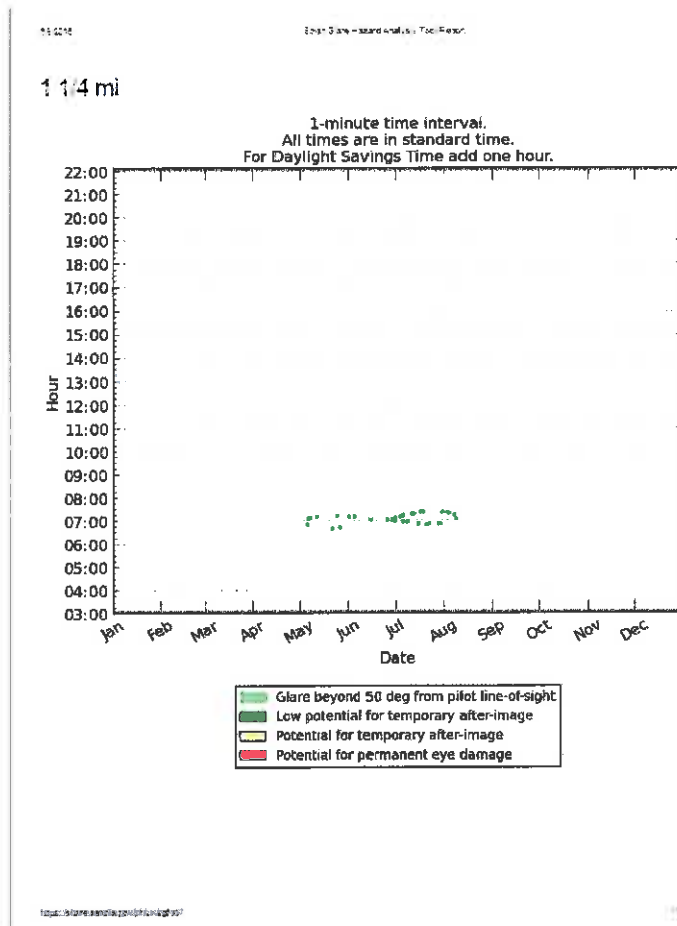
GLARE ANALYSIS



RUSSAK (REPRESENTATIVE NICK FULLERTON, FULLERTON ARCHITECTS)

NFA

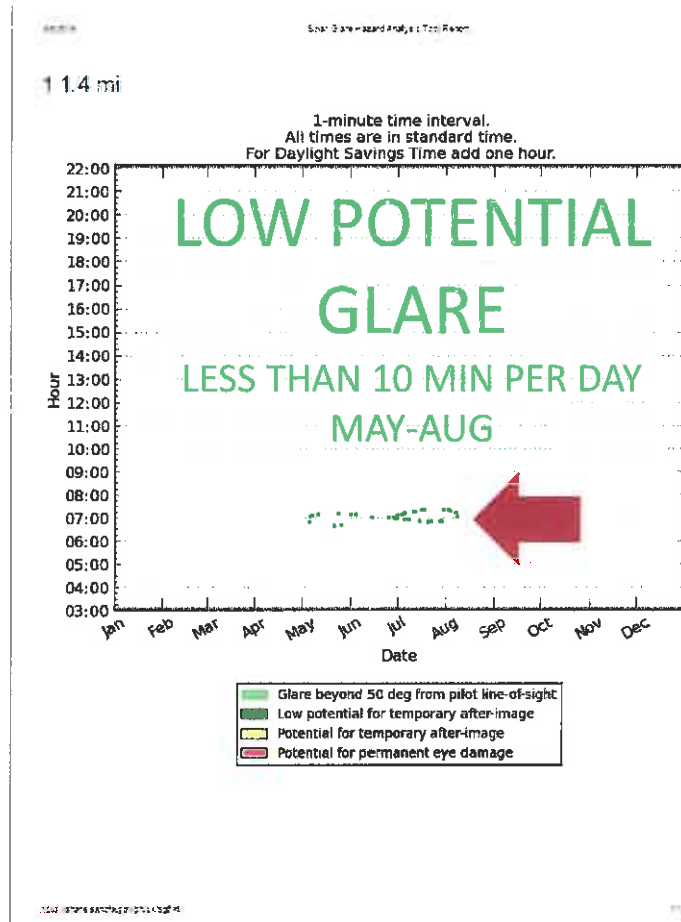
GLARE ANALYSIS



RUSSAK (REPRESENTATIVE NICK FULLERTON, FULLERTON ARCHITECTS)

NFA

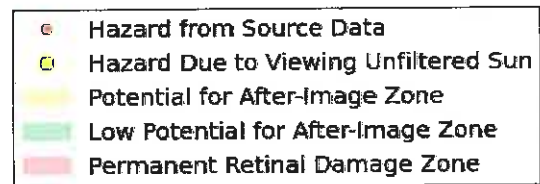
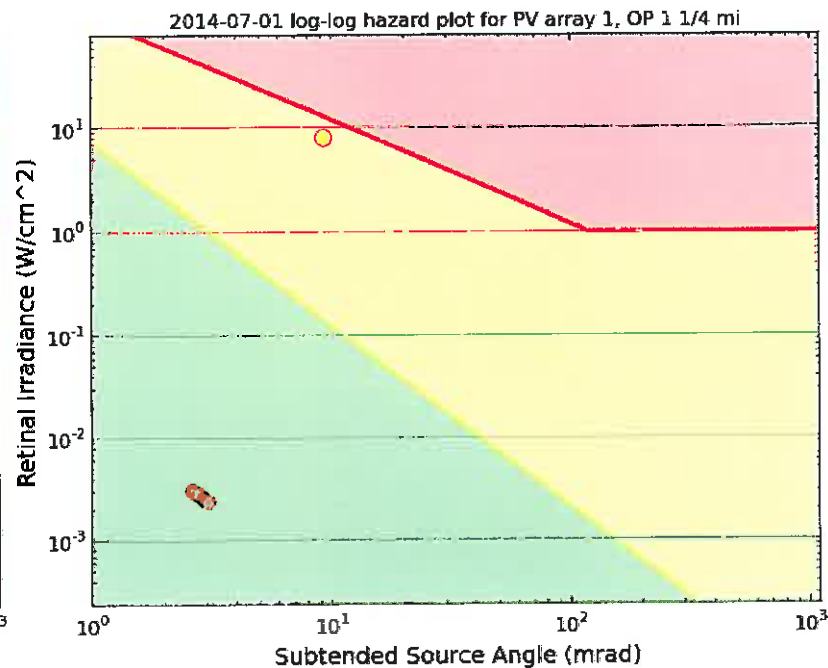
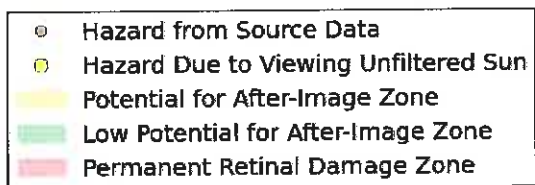
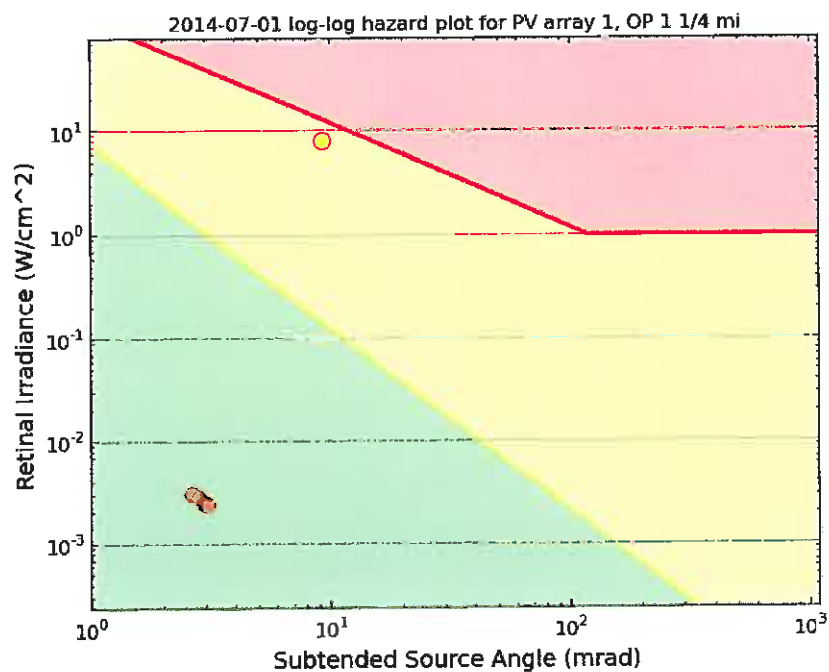
GLARE ANALYSIS



RUSSAK (REPRESENTATIVE NICK FULLERTON, FULLERTON ARCHITECTS)

NFA

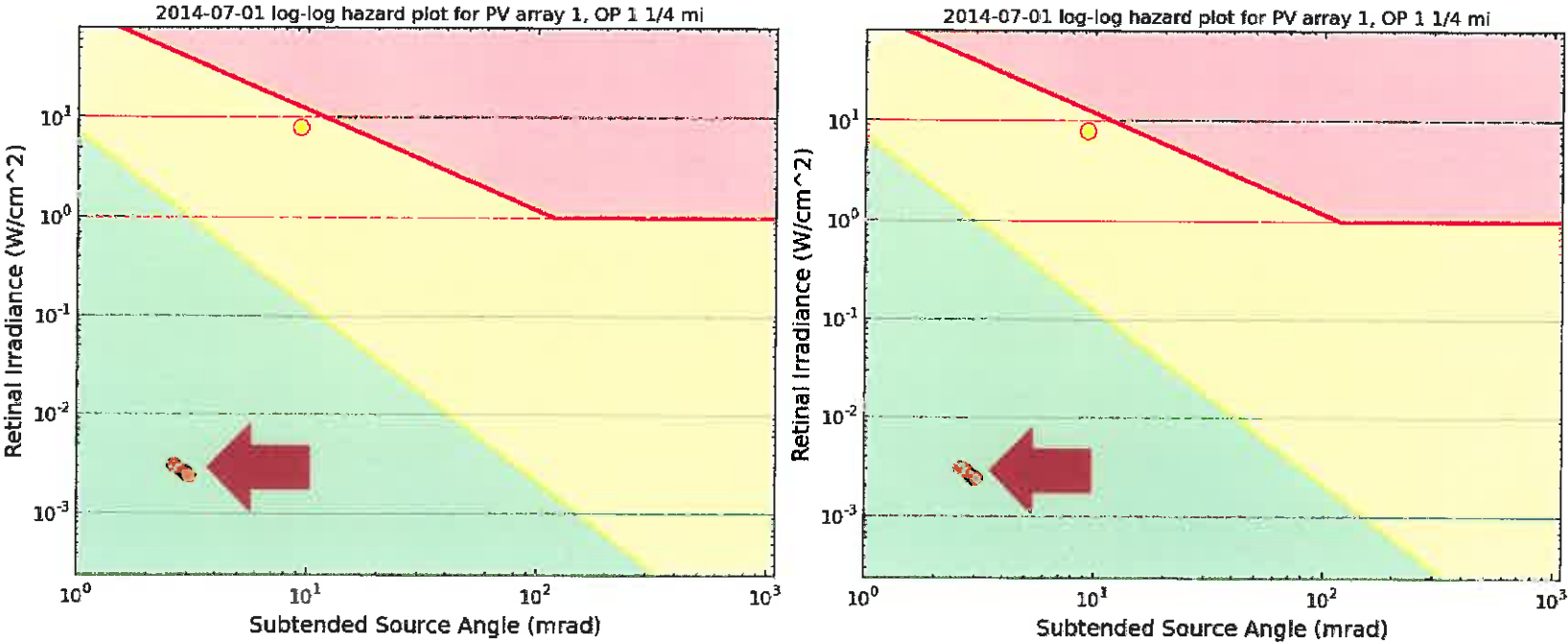
GLARE ANALYSIS



RUSSAK (REPRESENTATIVE NICK FULLERTON, FULLERTON ARCHITECTS)

NFA

GLARE ANALYSIS



- Hazard from Source Data
- Hazard Due to Viewing Unfiltered Sun
- Potential for After-Image Zone
- Low Potential for After-Image Zone
- Permanent Retinal Damage Zone

LOW POTENTIAL
GLARE

← FAA ALLOWABLE ZONE →

RUSSAK (REPRESENTATIVE NICK FULLERTON, FULLERTON ARCHITECTS)

NFA

GLARE ANALYSIS TOWER

144200 144200 Solar Glare Hazard Analysis Tool Report

Solar Glare Hazard Analysis Report

Generated June 8, 2015, 10:34 a.m.

No glare found

Print



Inputs

Analysis name	Russak
Priority axis tracking	none
Orientation of array (deg)	180.0
Tilt of solar panels (deg)	27.0
Rated power (kW)	0.0
Very reflectivity	True

144200 144200

11

RUSSAK (REPRESENTATIVE NICK FULLERTON, FULLERTON ARCHITECTS)

NFA

GLARE ANALYSIS TOWER

09/2016 Solar Glare Hazard Analysis Tool - Project

Solar Glare Hazard Analysis Report

Generated June 8, 2016, 10:34 a.m.

No glare found



Print



Inputs

Analysis name	RUSSAK
PV array axis tracking	none
Orientation of array (deg)	180.0
Tilt of solar panels (deg)	27.0
Rated power (kW)	0.0
Vary reflectivity	True

Task: Solar Glare Hazard Analysis

10

RUSSAK (REPRESENTATIVE NICK FULLERTON, FULLERTON ARCHITECTS)

NFA

NOTICE OF PUBLIC HEARING

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Thursday, from 8:00 a.m. to 5:00 p.m., except Monday, September 7 (Labor Day).

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon St., 1st Floor Hearing Room
Riverside, California

DATE OF HEARING: September 10, 2015

TIME OF HEARING: 9:00 A.M.

CASE DESCRIPTION:

ZAP1032TH15 – Fullerton Architects PC (Nick Fullerton), for Allen Russak – County Case No. BRS 150586 (Building Permit). The applicant proposes to install and operate roof mounted photovoltaic solar panels on a private member's seasonal residence/garage located at 61776 Fullerton Drive (Lot 202 of Thermal Club – Assessor's Parcel Number 759-220-028) within the motorsports facility located northerly of 62nd Avenue, easterly of Tyler Street, westerly of Polk Street, and southerly of 60th Avenue in the unincorporated community of Thermal (Zone D of the Jacqueline Cochran Regional Airport Influence Area).

FURTHER INFORMATION: Contact Russell Brady at (951) 955-0549 or John Guerin at (951) 955-0982. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions.

APPLICATION FOR MAJOR LAND USE ACTION REVIEW
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

ALUC Identification No.

ZAPI032THIS

PROJECT PROPONENT (TO BE COMPLETED BY APPLICANT)

Date of Application 07/07/2015
 Property Owner Allen Russak Phone Number 323-353-7357
 Mailing Address 7467 Dorie Drive
West Hills, CA 91307 7467

Agent (if any) Nick Fullerton Phone Number 406.837.1550
 Mailing Address PO Box 2770
Eagle Bend Drive
Bigfork, MT 59911

PROJECT LOCATION (TO BE COMPLETED BY APPLICANT)

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address 61776 Fullerton Drive
 Assessor's Parcel No. 759-220-028 Parcel Size 7,678sf
 Subdivision Name The Thermal Club Zoning Riverside County,
 Lot Number 202 Classification Specific Plan 303,
planning area E-6

PROJECT DESCRIPTION (TO BE COMPLETED BY APPLICANT)

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe) Vacant lot

Proposed Land Use (describe) Single Family Residential
with rooftop solar array

For Residential Uses Number of Parcels or Units on Site (exclude secondary units) 1

For Other Land Uses Hours of Use _____

(See Appendix C) Number of People on Site Maximum Number _____
 Method of Calculation _____

Height Data Height above Ground or Tallest Object (including antennas and trees) 27 ft.
 Highest Elevation (above sea level) of Any Object or Terrain on Site (353 + 27) 380 ft.

Flight Hazards Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight? Yes
 No

If yes, describe rooftop solar panels

J. Cochran
D

REFERRING AGENCY (APPLICANT OR JURISDICTION TO COMPLETE)

Date Received	_____	Type of Project
Agency Name	_____	<input type="checkbox"/> General Plan Amendment
Staff Contact	Russel Brady	<input type="checkbox"/> Zoning Amendment or Variance
Phone Number	_____	<input type="checkbox"/> Subdivision Approval
Agency's Project No.	Building Permit / Tenant Improv.	<input type="checkbox"/> Use Permit
		<input type="checkbox"/> Public Facility
		<input type="checkbox"/> Other _____

A. NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. SUBMISSION PACKAGE:

ALUC REVIEW

- 1 Completed Application Form
- 1 Project Site Plan – Folded (8-1/2 x 14 max.)
- 1 Elevations of Buildings - Folded
- 1 Each . 8 ½ x 11 reduced copy of the above
- 1 8 ½ x 11 reduced copy showing project in relationship to airport.
- 1 Set . Floor plans for non-residential projects
- 4 Sets . Gummed address labels of the Owner and representative (**See Proponent**).
- 1 Set . Gummed address labels of all property owners within a 300' radius of the project site. If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10), with ALUC return address.
- 4 Sets . Gummed address labels of the referring agency (City or County).
- 1 Check for Fee (See Item "C" below)

STAFF REVIEW (Consult with ALUC staff planner as to whether project qualifies)

- 1 Completed Application Form
- 1 Project Site Plans – Folded (8-1/2 x 14 max.)
- 1 Elevations of Buildings - Folded
- 1 8 ½ x 11 Vicinity Map
- 1 Set . Gummed address labels of the Owner and representative (**See Proponent**).
- 1 Set . Gummed address labels of the referring agency.
- 1 Check for review—See Below

■ nfa ■
fullerton
architects, pc

July 28, 2015

Russell Brady
Riverside County Airport Land Use Commission
4080 Lemon Street, 14th Floor
Riverside, CA 92501

RE: Solar Application
Russak/Lot 202
Building Permit # BRS150586

Hi Russell,

Attached are the gummed labels for the 300' radius surrounding property owners and owner/representative. Thermal Operating Company, LLC is the only property owner within the 300' radius of the subject property.

The building permit number is BRS150586 and the Riverside County Planning contact person is Ken Biaz.

I believe Tyler Fullerton, from our LA office, is answering your other concerns. If there's anything else you need on our end, please let Tyler know and we will get it taken care of.

Thanks for your help on this.



Carol Fullerton
Corporate Secretary
Fullerton Architects, PC

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.5

HEARING DATE: September 10, 2015

CASE NUMBER: ZAP1059BD15 – Wagner Architecture Group, Inc. for GEOVEL, Inc. (Representative: Jessica Peat or Walt Wagner)

APPROVING JURISDICTION: City of Indio

JURISDICTION CASE NO: CUP15-6-009 (Conditional Use Permit); DR15-5-387 (Design Review)

MAJOR ISSUES: While the property is split between Airport Compatibility Zones B1, B2, and D, the primary building is split between Airport Compatibility Zones B1 and B2, with much of the most intensely occupied area as evaluated through the Building Code within Airport Compatibility Zone B1, where non-residential intensity is limited to an average of 25 persons per acre and a single-acre maximum of 50 persons (prior to application of density bonuses). The applicant team has not provided a breakdown of building occupancy in a manner that separates occupancy between the portions of the building in Compatibility Zone B1 and the portion in Compatibility Zone B2. The project would not qualify for consideration as Infill pursuant to Countywide Policy 3.3.1 since this Policy is not applicable to Compatibility Zone B1. Even if it were, less than 65% of the project's perimeter is bordered by projects of similar intensity. There are similar projects farther to the west in Compatibility Zone B1 (westerly of Adams Street). Staff contacted the applicant, who, at this time, is unwilling to alter the proposed building location.

RECOMMENDATION: Staff must recommend a finding of INCONSISTENCY for the Conditional Use Permit/Design Review as proposed due to the single-acre intensity, but would encourage a continuance if the applicant changes his mind and agrees to consider locating the proposed facility elsewhere on the project site (specifically outside the boundaries of Airport Compatibility Zone B1).

PROJECT DESCRIPTION: The applicant proposes to establish an automobile dealership (Chrysler Dodge Jeep Ram) within a 4-acre portion of a 17.94-acre parcel. The dealership would include a 41,413 square foot main building (36,921 square feet excluding canopy) and a 1,300 square foot car wash. An additional 12,115 square foot building and outdoor parking spaces are proposed on an adjacent 4.95 acres of the same parcel.

PROJECT LOCATION: The site is located on the northerly side of Varner Road, easterly of its intersection with Adams Street, in the City of Indio, approximately 1,196 feet north/northwesterly of

the westerly terminus of Runway 10-28 at Bermuda Dunes Airport.

LAND USE PLAN: 2004 Bermuda Dunes Airport Land Use Compatibility Plan

- a. Airport Influence Area: Bermuda Dunes Airport
- b. Land Use Policy: Compatibility Zones B1, B2, and D
- c. Noise Levels: 60-65 CNEL in area proposed for development

BACKGROUND:

Non-Residential Average Intensity: Compatibility Zone B1 limits average intensity to 25 people per acre. If outdoor areas (other than areas under the canopy used for service reception) are excluded, the applicant indicates an occupancy of 270 for the main building using the Building Code, prior to the 50 percent reduction. Applying a 50 percent reduction across-the-board brings the total occupancy of the main building down to 135. If we further assume that the secondary 12,115 square foot building is a business use, this would add an additional 61 persons with the 50 percent reduction, for a cumulative total of 196. Given that the site is 17.94 acres in area, this would result in an average intensity of 11 persons per acre if spread across the site.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle in the absence of more precise data). However, this method does not work for an automotive dealership, where the vast majority of parking spaces are for the vehicles for sale or vehicles being serviced.

Non-Residential Single-Acre Intensity: Single-acre intensity limits have been adopted for Airport Land Use Compatibility Plan zones to protect people on the ground and minimize risk of casualties in the event of an aircraft accident. Compatibility Zone B1 limits single-acre intensity to 50 people. Projects qualifying for a full 30 percent risk-reduction bonus could potentially be permitted a single-acre intensity up to 65 persons. As noted, the applicant team has not provided a breakdown of building occupancy in a manner that separates occupancy between the portions of the building in Airport Compatibility Zone B1 and the portion in Airport Compatibility Zone B2. The exhibits as submitted do not depict the location of the Compatibility Zone boundary, but, based on staff's "best guess" of the locations of these zones on the property, both the showroom/sales area and "Main Street" appear to be wholly within Compatibility Zone B1. If this is, in fact, the case, then the single-acre intensity is inconsistent in that the combined occupancy of these areas is 116 (58 with the 50 percent reduction).

Risk-Reduction Design Bonus: A bonus of up to 30% above the single-acre criterion of 50 for a maximum allowable intensity of 65 could be granted at the authority of the City of Indio based on the type and amount of risk reduction measures incorporated. This makes the provision of information

regarding areas of the building within Zone B1 vis-à-vis areas within Zone B2 critical information. Potential risk-reduction measures would be those identified in the Countywide Policies section of the 2004 Riverside County Airport Land Use Compatibility Plan. (An excerpt is attached.) No information has been provided to support the granting of a risk-reduction design bonus for this project.

Infill Potential: Pursuant to Countywide Policy 3.3.1, higher intensity criteria may be considered for projects in zones other than Compatibility Zones A and B1 if the surrounding land uses are similar to or more intense than the proposed project. To qualify for consideration, at least 65% of the project site's perimeter must be surrounded by uses similar to or more intense than the proposed project, and the project site must be less than 20 acres in area. If qualified, a higher average intensity level - the lesser of either the equivalent intensity to surrounding land uses or double the normally allowable intensity - may be consistent. The proposed structure would be split between Airport Compatibility Zones B1 and B2.

The area to the west along the northerly side of Varner Road within Airport Compatibility Zone B1 includes automobile dealerships of similar size. Historical aerial photos reveal that the dealerships were built between 1996 and 2004.

Prohibited and Discouraged Uses: The applicant does not propose any uses specifically prohibited or discouraged in Zone B1 (Children's schools, day care centers, libraries, hospitals, nursing homes, buildings with more than 2 aboveground habitable floors, highly noise-sensitive outdoor non-residential uses, aboveground bulk storage of hazardous materials, critical community infrastructure facilities, and hazards to flight) within the project. Projects within Zone B1 are encouraged to locate structures a maximum distance from the extended runway centerline. The extended runway centerline for Bermuda Dunes Airport would be southerly of Interstate 10 at this longitude.

Noise: The structure is proposed to be sited within the area that would be subject to average exterior noise levels from aircraft operations of 60-65 CNEL under ultimate airport development conditions. As a non-residential use that is not especially noise sensitive, no special mitigation of noise from aircraft is recommended, especially since the site is subject to a greater noise level from the continuous traffic on Interstate 10 directly southerly of Varner Road.

Part 77: The elevation of Runway 10-28 at its westerly terminus is approximately 73 feet above mean sea level (AMSL). At a distance of approximately 1,196 feet from the runway, FAA review would be required for any structures with top of roof exceeding 84 feet AMSL. The finished floor elevation of the dealership is 77.2 feet AMSL and its height is 30 feet, for a maximum elevation of 107.2 feet AMSL. Therefore, Federal Aviation Administration (FAA) obstruction evaluation review is required.

Open Area: The area proposed for development is less than ten acres in area, but the overall property is 17.94 acres in area; therefore, the project is subject to open area requirements of the Airport Compatibility Zones in which it is located (30 percent in Compatibility Zone B1 and 10 percent in

Compatibility Zone D). At this time, more than 50 percent of the overall property would remain open area.

Commentary: The proponent has noted that there are automobile dealerships of similar size located along Varner Road within Airport Compatibility Zone B1 westerly of this site. However, in those cases, the entire property was located within Zone B1. In this case, the parcel in question is split between Airport Compatibility Zones B1, B2, and D. While the area in Compatibility Zone D is somewhat removed from the frontage of Varner Road, there appears to be sufficient land area within Airport Compatibility Zone B2 (within the site, but easterly of the proposed primary building location) that a “swap” of the primary and secondary building locations could result in the primary building being sited completely outside the boundaries of Compatibility Zone B1. The proponent has indicated a preference for maintaining the proposed location for the primary building and seeking an overrule from the City Council.

It should be noted that the requirement for notification to the Federal Aviation Administration Obstruction Evaluation Service through the Form 7460-1 online process at oaaaa.faa.gov is independent of the ALUC review process and required by Part 77 of the Federal Aviation Regulations when the elevation at the top point of a structure exceeds the elevation of a runway within 20,000 feet by more than one foot for every 100 feet of distance from a runway whose length exceeds 3,200 feet.

CONDITIONS:

1. Any outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky.
2. The following uses are prohibited:
 - (a) Any use or activity which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use or activity which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use or activity which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area.
 - (d) Any use which would generate electrical interference that may be detrimental to the

operation of aircraft and/or aircraft instrumentation.

3. The following uses are specifically prohibited in Airport Compatibility Zone B1: Day care centers, children's schools, places of worship, libraries, hospitals, nursing homes, highly noise-sensitive outdoor non-residential uses, aboveground bulk storage of hazardous materials, critical community infrastructure facilities, and hazards to flight.
4. Prior to issuance of building permits, the landowner shall convey an avigation easement to Bermuda Dunes Executive Airport.
5. The attached notice shall be provided to all potential purchasers of the property and/or tenants of the proposed buildings.
6. Any new detention basin(s) on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
7. Prior to issuance of a building permit, the permittee shall submit Form 7460-1 online to the Federal Aviation Administration Obstruction Evaluation Service, and shall have received a Determination of No Hazard to Air Navigation.

Note: Compliance with the above conditions does not render the project consistent with the Compatibility Plan. These provisions are necessary to prevent the project from negatively impacting the airport or aircraft operations.

- 4.2.5. *Limitations on Clustering:* Policy 4.2.4(d) notwithstanding, limitations shall be set on the maximum degree of clustering or usage intensity acceptable within a portion of a large project site. These criteria are intended to limit the number of people at risk in a concentrated area.
- (a) Clustering of new residential development shall be limited as follows:
 - (1) Within *Compatibility Zone A*, clustering is not applicable.
 - (2) Within *Compatibility Zones B1, B2, and C*, no more than 4 dwelling units shall be allowed in any individual acre. Buildings shall be located as far as practical from the extended runway centerline and normal aircraft flight paths.
 - (b) Unless special design measures as listed in Policy 4.2.6 are utilized, usage intensity of new nonresidential development shall be limited as follows:
 - (1) Within *Compatibility Zone A*, clustering is not applicable.
 - (2) Within *Compatibility Zone B1*, uses shall be limited to a maximum of 50 people per any individual acre (i.e., a maximum of double the average intensity criterion set in Table 2A). Theaters, restaurants, most shopping centers, motels, intensive manufacturing or office uses, and other similar uses typically do not comply with this criterion.
 - (3) Within *Compatibility Zone B2*, uses shall be limited to a maximum of 200 people per any individual acre (i.e., a maximum of double the average intensity criterion set in Table 2A). Theaters, major shopping centers (500,000 or more square feet), large motels and hotels with conference facilities, and similar uses typically do not comply with this criterion.
 - (4) Within *Compatibility Zone C*, uses shall be limited to a maximum of 150 people per any individual acre (i.e., a maximum of double the average intensity criterion set in Table 2A). Theaters, fast-food establishments, high-intensity retail stores or shopping centers, motels and hotels with conference facilities, and similar uses typically do not comply with this criterion.
 - (5) Within *Compatibility Zone D*, uses shall be limited to a maximum of 300 people per any individual acre (i.e., a maximum of triple the average intensity criterion set in Table 2A).
 - (c) For the purposes of the above policies, the one-acre areas to be evaluated shall be rectangular (reasonably close to square, not elongated or irregular) in shape.
 - (d) In no case shall a proposed development be designed to accommodate more than the total number of dwelling units per acre (for residential uses) or people per acre (for nonresidential uses) indicated in Table 2A times the gross acreage of the project site. A project site may include multiple parcels. Appendix D lists examples of the types of land uses which are potentially compatible under these criteria and the types of land uses which are considered incompatible.
- 4.2.6. *Risk Reduction Through Building Design:* The number of people permitted to occupy a single nonresidential building may be increased by a factor of up to 1.3 times the limitations set by the preceding policy on clustering if special measures are taken to reduce the risks to building occupants in the event that the building is struck by an aircraft.

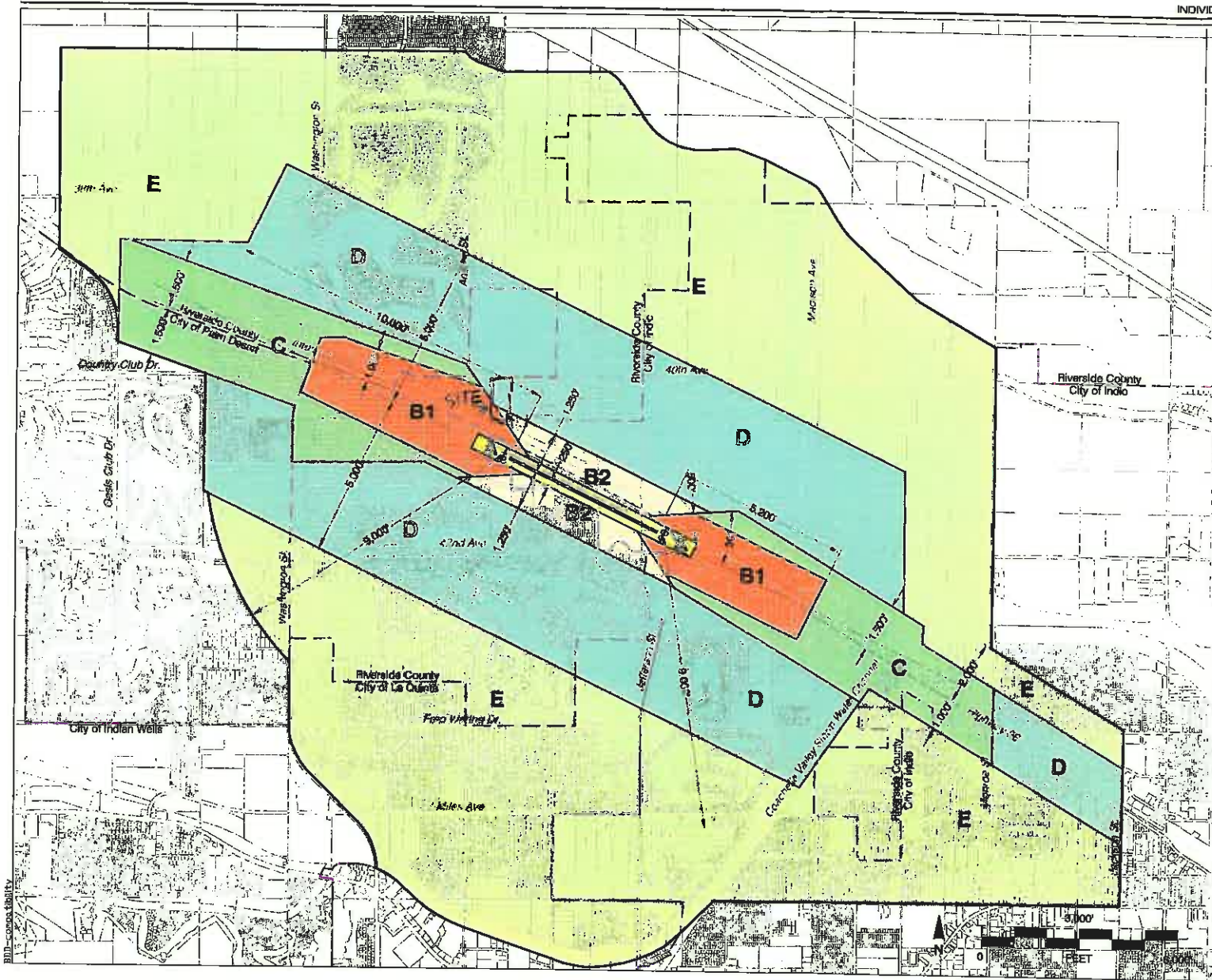
- (a) This intensity bonus is not applicable within *Compatibility Zone A* (no buildings are permitted) or *E* (densities and intensities are not limited) and shall not be applied to buildings situated within *Compatibility Zones B1, B2, or C* for runways routinely used by large aircraft (aircraft having a maximum certificated takeoff weight of more than 12,500 pounds).
- (b) Building design features which would enable application of an intensity bonus include, but are not limited to, the following:
 - › Using concrete walls;
 - › Limiting the number and size of windows;
 - › Upgrading the strength of the building roof;
 - › Avoiding skylights;
 - › Enhancing the fire sprinkler system;
 - › Limiting buildings to a single story; and
 - › Increasing the number of emergency exits.
- (c) Project proponents who wish to request an intensity bonus must include appropriate details of the building design along with their project review application.
- (d) Intensity bonuses shall be considered and approved by affected local jurisdictions on a case-by-case basis. The criteria to be used by each jurisdiction when considering intensity bonus requests shall be reviewed and approved by the ALUC as part of the general plan consistency process or subsequent action.

4.3. Airspace Protection

- 4.3.1. *Policy Objective:* Tall structures, trees, and other objects, particularly when located near airports or on high terrain, may constitute hazards to aircraft in flight. Federal regulations establish the criteria for evaluating potential obstructions. These regulations also require that the Federal Aviation Administration be notified of proposals for creation of certain such objects. The FAA conducts “aeronautical studies” of these objects and determines whether they would be hazards, but it does not have the authority to prevent their creation. The purpose of ALUC airspace protection policies, together with regulations established by local land use jurisdictions and the state government, is to ensure that hazardous obstructions to the navigable airspace do not occur.
- 4.3.2. *Basis for Height Limits:* The criteria for limiting the height of structures, trees, and other objects in the vicinity of an airport shall be based upon: Part 77, Subpart C, of the Federal Aviation Regulations (FAR); the United States Standard for Terminal Instrument Procedures (TERPS); and applicable airport design standards published by the Federal Aviation Administration. Airspace plans depicting the critical areas for airspace protection around each of the airports covered by this *Compatibility Plan* are depicted in Chapter 3.
- 4.3.3. *ALUC Review of Height of Proposed Objects:* Based upon FAA criteria, proposed objects that would exceed the heights indicated below for the respective compatibility zones potentially represent airspace obstructions issues. Development proposals that include any such objects shall be reviewed by the ALUC. Objects of lesser height normally would not have a potential for being airspace obstructions and therefore do

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Legend

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E

Boundary Lines

- Airport Property Line
- City Limits

Note

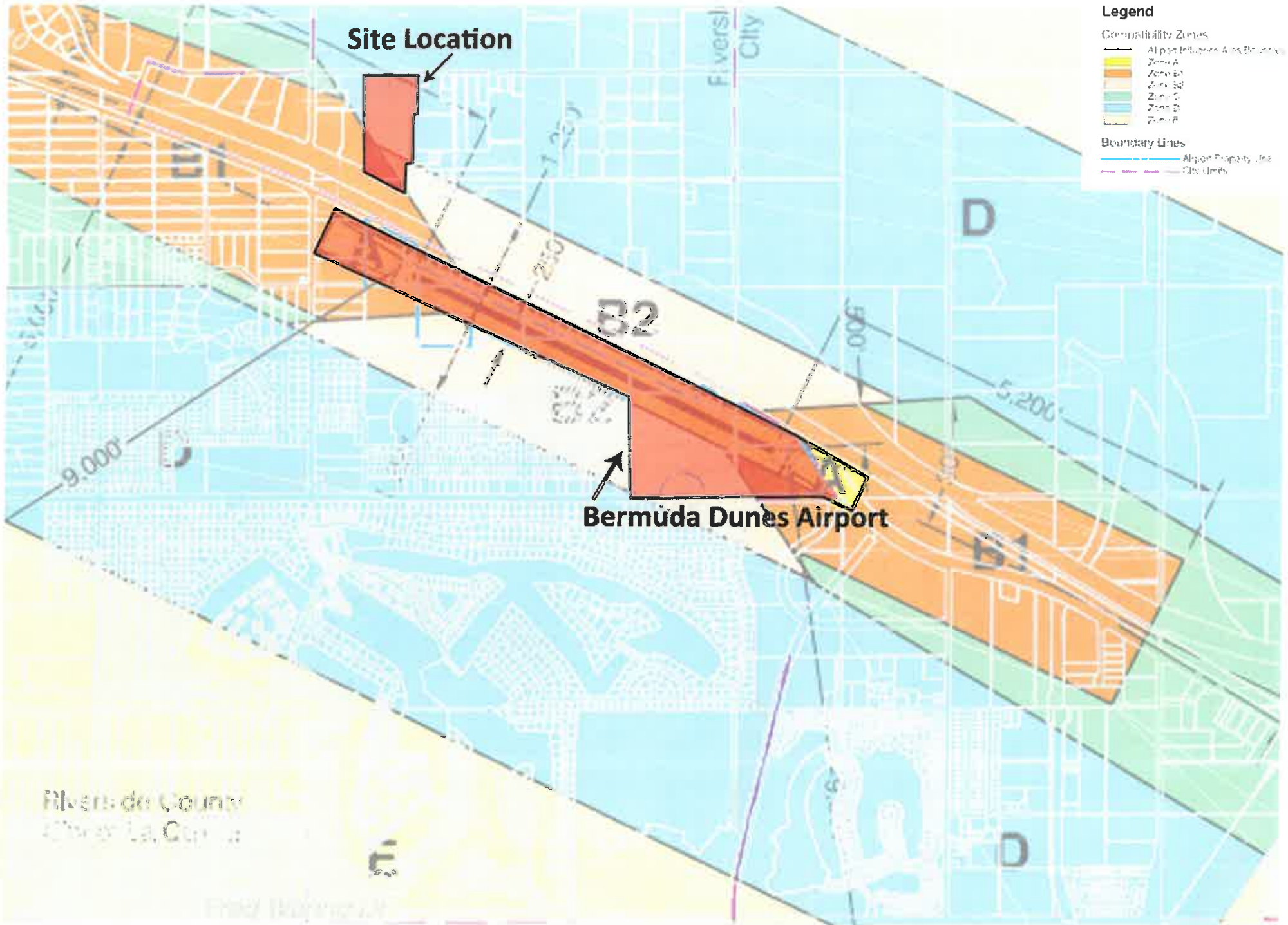
Southwestern edge of Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

See Chapter 2, Table 2A for compatibility criteria associated with this map.

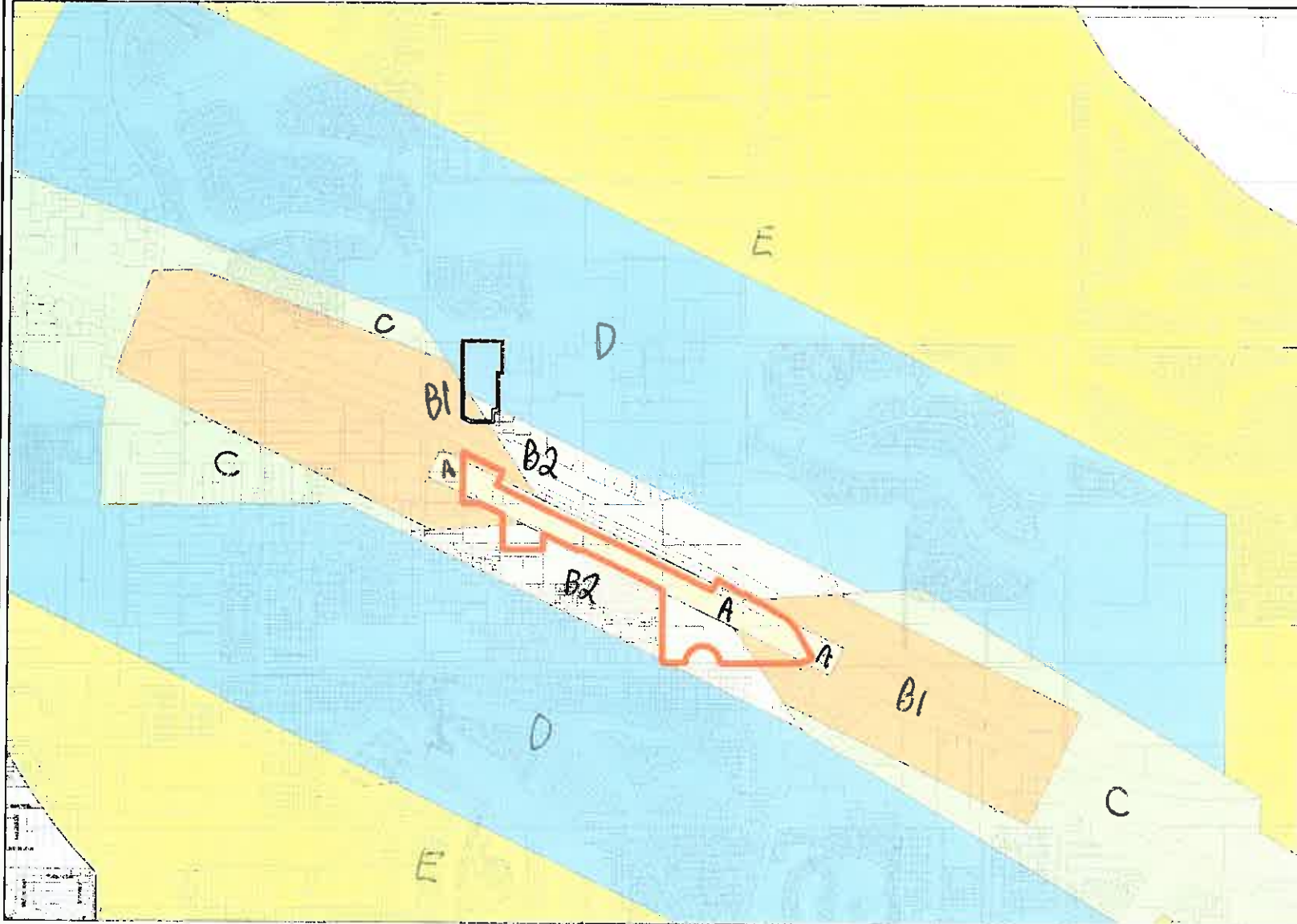
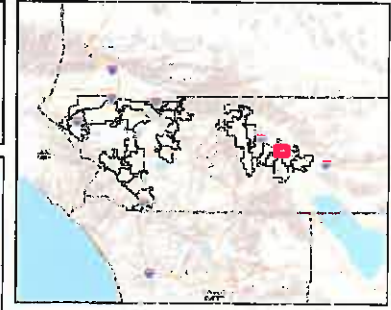
Riverside County
Airport Land Use Commission
Riverside County
Airport Land Use Compatibility Plan
Policy Document
(Adopted December 2004)

Map BD-1

Compatibility Map
Bermuda Dunes Airport



My Map

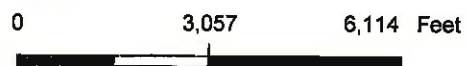


Legend

- Airports
- Airport Compatibility**
- OTHER_ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT
- C2-HIGHT-EXC1



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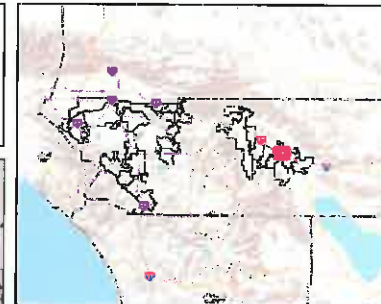


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Notes

My Map



Legend

- City Boundaries
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



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0 3,057 6,114 Feet

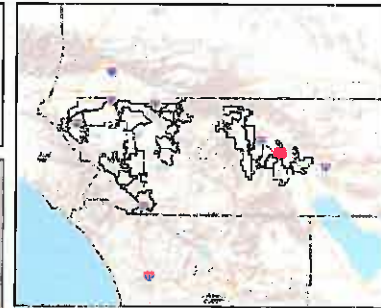


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Notes

My Map



Legend

-  RCLIS Parcels
-  City Boundaries
- roadsanno
- highways
-  HWY
-  INTERCHANGE
-  INTERSTATE
-  OFFRAMP
-  ONRAMP
-  USHWY
-  counties
-  cities
- hydrographylines
- waterbodies
-  Lakes
-  Rivers



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0 1,529 3,057 Feet

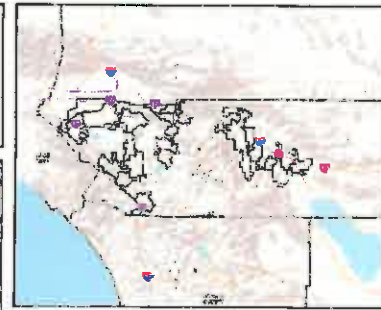


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Notes

My Map

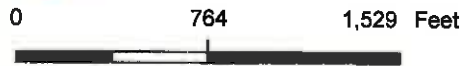


Legend

- RCLIS Parcels
- City Boundaries
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



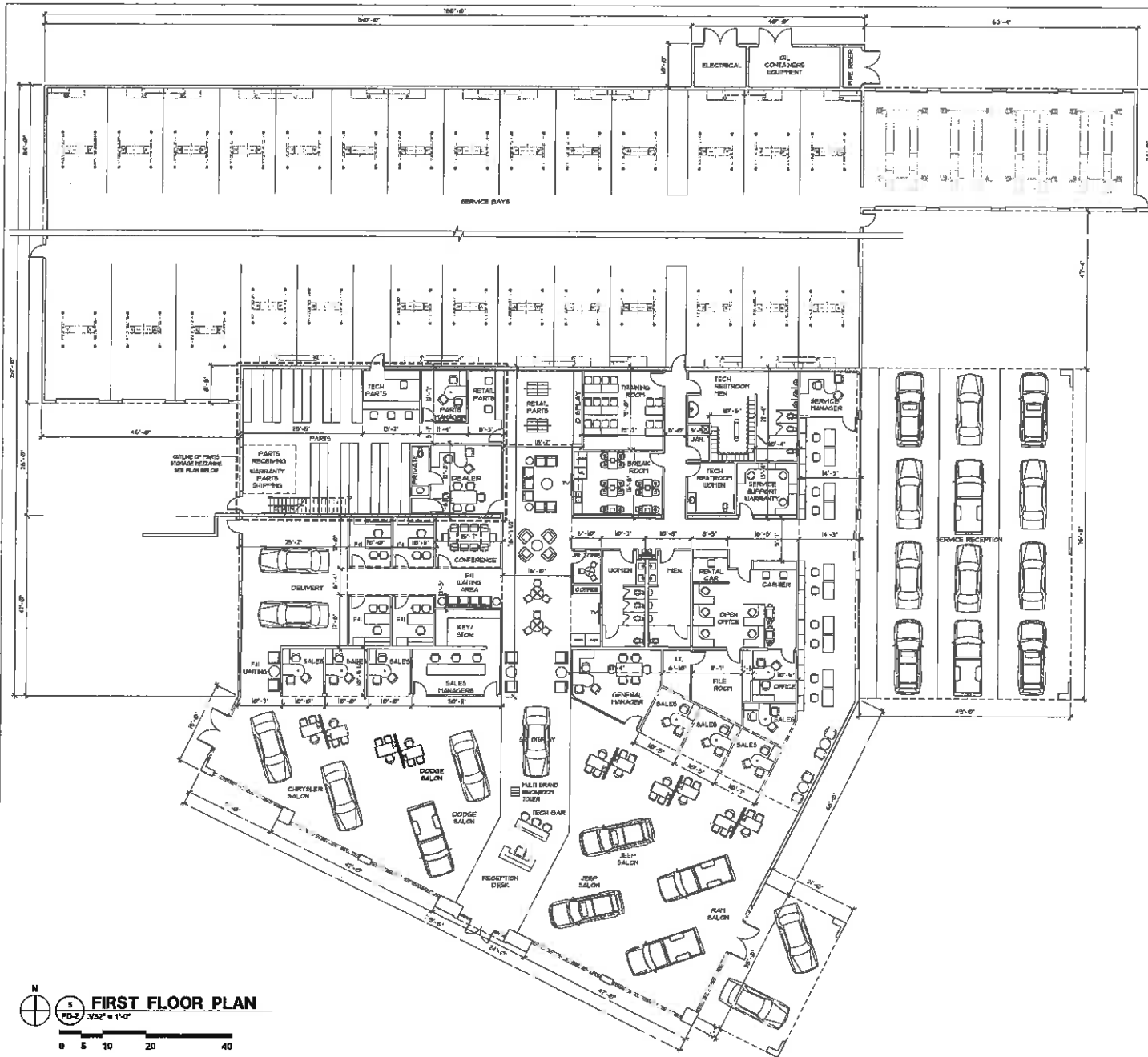
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.



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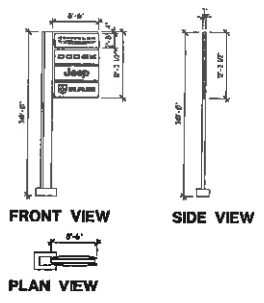
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Notes

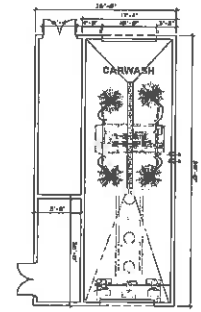


5 FIRST FLOOR PLAN
PD-2 3/32" = 1'-0"

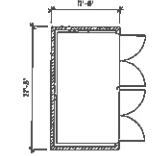
0 5 10 20 40



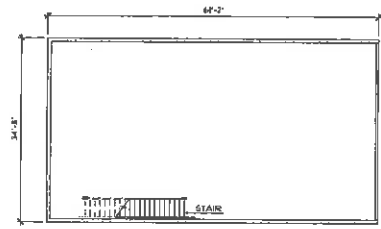
1 CDJR BRAND SIGN
3/32" = 1'-0"



2 FLOOR PLAN
3/32" = 1'-0" CARWASH



3 FLOOR PLAN
3/32" = 1'-0" TRASH ENCLOSURE



4 PARTS STORAGE SECOND FLOOR PLAN
3/32" = 1'-0"

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Fax: (435) 224-7500
http://www.wagnerarch.com

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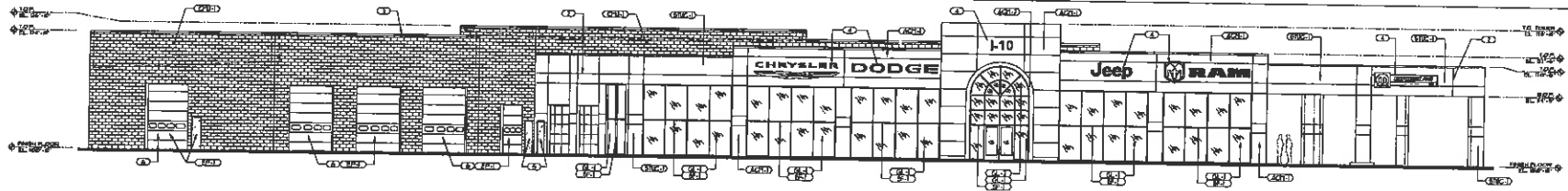
99-200 Varner Road
Indio, CA 92201



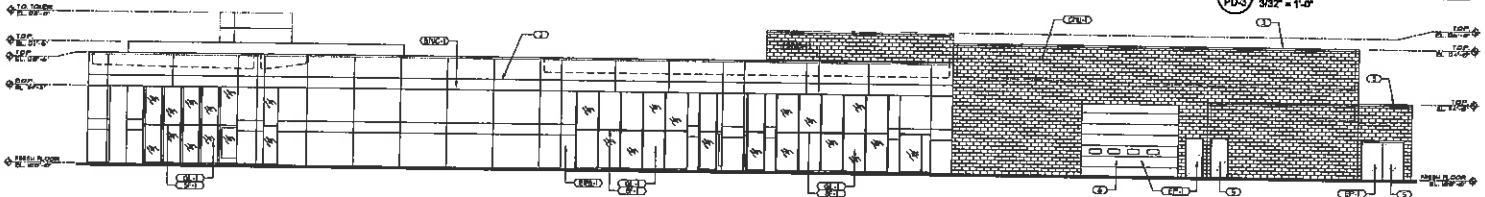
PROPOSED SALES FLOOR PLAN

DATE	
SCALE	
PROJECT	
NO. OF SHEETS	
NO. OF SHEETS USED	
DATE	
BY	
CHECKED BY	
DATE	

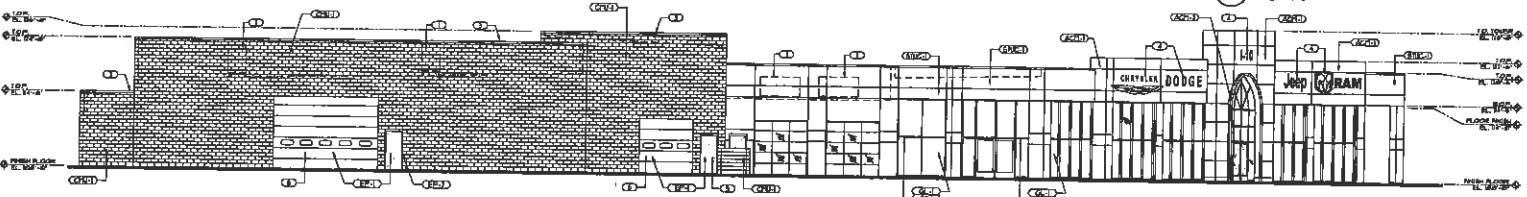
PD-2



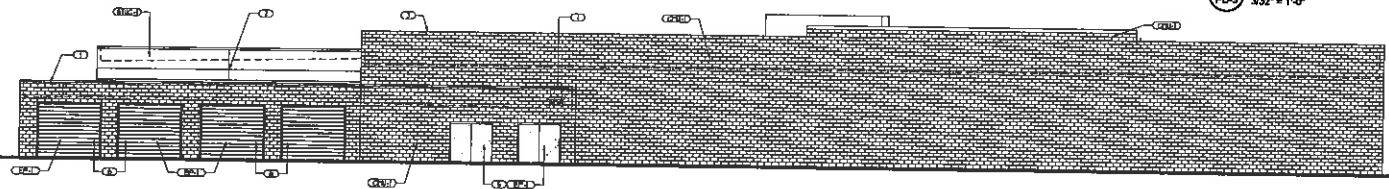
1 SOUTH ELEVATION
PD-3 3/32" = 1'-0"



2 EAST ELEVATION
PD-3 3/32" = 1'-0"



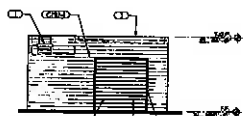
3 WEST ELEVATION
PD-3 3/32" = 1'-0"



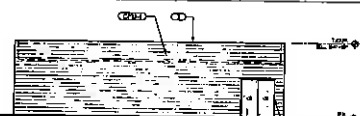
4 NORTH ELEVATION
PD-3 3/32" = 1'-0"



5 EAST ELEVATION
PD-3 3/32" = 1'-0" CARWASH



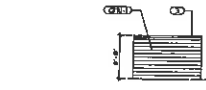
6 NORTH ELEVATION
PD-3 3/32" = 1'-0" CARWASH



7 WEST ELEVATION
PD-3 3/32" = 1'-0" CARWASH



8 SOUTH ELEVATION
PD-3 3/32" = 1'-0" CARWASH



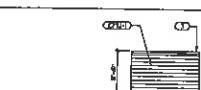
9 NORTH ELEVATION
PD-3 3/32" = 1'-0" TRASH ENCLOSURE



10 WEST ELEVATION
PD-3 3/32" = 1'-0" TRASH ENCLOSURE



11 EAST ELEVATION
PD-3 3/32" = 1'-0" TRASH ENCLOSURE



12 SOUTH ELEVATION
PD-3 3/32" = 1'-0" TRASH ENCLOSURE

EXTERIOR FINISH SCHEDULE	
CR01	ALUMINUM COMPOSITE MATERIAL AL POLYS BRASH SILVER METALLIC DRG. DRG. 301111
CR02	ALUMINUM COMPOSITE MATERIAL AL POLYS BRASH SILVER METALLIC DRG. DRG. 301111
CR03	BRICK 1/4" THICK REVERSED COMB BAND FINEGR. COLO. DR. 075521 1992
CR04	ROUNDER POORER PAINT COLOR POLYGLASS GRAY 803 1991 1991
CR05	EXTERIOR 1/4" THICK SOLIDWITRORE PAINT COLOR: NOCK GRAY 803 1991 1991
CR06	HOLLOW GLASS BRUSH MANUFACTURER: PPG LOW E CLEAR GLASS
CR07	HOLLOW GLASS BRUSH MANUFACTURER: PPG LOW E CLEAR GLASS
CR08	EXTERIOR 1/4" THICK SOLIDWITRORE PAINT COLOR: NOCK GRAY 803 1991 1991
CR09	CONCRETE TAMPONED UNIT BRICK FINISH: STRENGTHENED BRUIFF FACE BLOCK, NO ADHESIVE MATERIAL EXTERIOR PAINT COLOR: 8711

KEY ELEVATIONS NOTES	
NOTES ARE GENERAL - NOT ALL ITEMS INDICATED ON EACH	
CR	MECHANICAL UNIT - APPROPRIATE LOCATION ONLY
CD	MECHANICAL UNIT
CC	PREPARED FIELD, FINISHING TO MATCH EP-4
CE	APPROPRIATE LOCATION FOR RITING SIGN - PROVIDE ELECTRICAL UNITS FOR THESE SIGN AT DISCRETE OF WALL
CD	HOLLOW UNIT, DOOR
CE	OVERHEAD SECTIONAL DOOR

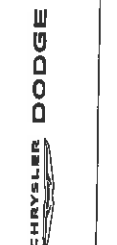
GRAPHIC SCALE 3/32" = 1'-0"	
0	5 10 20 40

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ARCHITECTS
113 Park Drive Drive, Suite 101
Channah, CA 92584
Phone: 714.772.2133
Fax: 714.772.2133
WagnerArchitect.com

THE ARCHITECT HAS PREPARED THESE ELEVATIONS FOR THE ARCHITECT'S CLIENT. THE ARCHITECT HAS NOT CONDUCTED A VISUAL CHECK OF THE ELEVATIONS AGAINST THE EXISTING BUILDING. THE ARCHITECT HAS NOT CONDUCTED A VISUAL CHECK OF THE ELEVATIONS AGAINST THE EXISTING BUILDING. THE ARCHITECT HAS NOT CONDUCTED A VISUAL CHECK OF THE ELEVATIONS AGAINST THE EXISTING BUILDING.



I-10 CDJR
99-200 Varner Road
Indio, CA 92201



PROPOSED SALES ELEVATIONS

DATE: _____
BY: _____
CHECKED BY: _____
DATE: _____

PD-3

- 3.2.2. *Establishment of Review Process:* Provisions must be made for evaluation of proposed land use development situated within an airport influence area relative to the compatibility criteria set forth in the *Compatibility Plan*.
- (a) Even if the land use designations in a general plan have been deemed consistent with the *Compatibility Plan*, evaluation of the proposed development relative to the land use designations alone is usually insufficient. General plans typically do not contain the detailed airport land use compatibility criteria necessary for a complete compatibility evaluation of proposed development.
 - (b) Local jurisdictions have the following choices for satisfying this evaluation requirement:
 - (1) Sufficient detail can be included in the general plan and/or referenced implementing ordinances and regulations to enable the local jurisdiction to assess whether a proposed development fully meets the compatibility criteria specified in the applicable compatibility plan (this requires both that the compatibility criteria be identified and that project review procedures be described);
 - (2) The ALUC's compatibility plan can be adopted by reference (in this case, the project review procedure must be described in a separate instrument presented to and approved by the ALUC); and/or
 - (3) The general plan can indicate that all major land use actions, as listed in Policy 1.5.3 or otherwise agreed to by the ALUC, shall be referred to the Commission for review in accordance with the policies of Section 2.3.

3.3. Special Conditions

- 3.3.1. *Infill:* Where development not in conformance with the criteria set forth in this *Compatibility Plan* already exists, additional infill development of similar land uses may be allowed to occur even if such land uses are to be prohibited elsewhere in the zone. This exception does not apply within *Compatibility Zones A* or *B1*.
- (a) A parcel can be considered for *infill* development if it meets *all* of the following criteria plus the applicable provisions of either Sub-policy (b) or (c) below:
 - (1) The parcel size is no larger than 20.0 acres.
 - (2) At least 65% of the site's perimeter is bounded (disregarding roads) by existing uses similar to, or more intensive than, those proposed.
 - (3) The proposed project would not extend the perimeter of the area defined by the surrounding, already developed, incompatible uses.
 - (4) Further increases in the residential density, nonresidential usage intensity, and/or other incompatible design or usage characteristics (e.g., through use permits, density transfers, addition of second units on the same parcel, height variances, or other strategy) are prohibited.
 - (5) The area to be developed cannot previously have been set aside as open land in accordance with policies contained in this *Plan* unless replacement open land is provided within the same compatibility zone.
 - (b) For residential development, the average development density (dwelling units per gross acre) of the site shall not exceed the lesser of:

- (1) The average density represented by all existing lots that lie fully or partially within a distance of 300 feet from the boundary of the parcel to be divided; or
 - (2) Double the density permitted in accordance with the criteria for that location as indicated in the Compatibility Criteria matrix, Table 2A.
- (c) For nonresidential development, the average usage intensity (the number of people per gross acre) of the site's proposed use shall not exceed the lesser of:
- (1) The average intensity of all existing uses that lie fully or partially within a distance of 300 feet from the boundary of the proposed development; or
 - (2) Double the intensity permitted in accordance with the criteria for that location as indicated in the Compatibility Criteria matrix, Table 2A.
- (d) The single-acre and risk-reduction design density and intensity multipliers described in Policies 4.2.5 and 4.2.6 and listed in Table 2A are applicable to infill development.
- (e) Infill development on some parcels should not enable additional parcels to then meet the qualifications for infill. The ALUC's intent is that parcels eligible for infill be determined just once. Thus, in order for the ALUC to consider proposed development under these infill criteria, the entity having land use authority (Riverside County or affected cities) must first identify the qualifying locations in its general plan or other adopted planning document approved by the ALUC. This action may take place in conjunction with the process of amending a general plan for consistency with the ALUC plan or may be submitted by the local agency for consideration by the ALUC at the time of initial adoption of this *Compatibility Plan*. In either case, the burden for demonstrating that a proposed development qualifies as infill rests with the affected land use jurisdiction and/or project proponent.
- 3.3.2. *Nonconforming Uses:* Existing uses (including a parcel or building) not in conformance with this *Compatibility Plan* may only be expanded as follows:
- (a) Nonconforming residential uses may be expanded in building size provided that the expansion does not result in more dwelling units than currently exist on the parcel (a bedroom could be added, for example, but a separate dwelling unit could not be built). No ALUC review of such improvements is required.
 - (b) A nonconforming nonresidential development may be continued, leased, or sold and the facilities may be maintained or altered (including potentially enlarged), provided that the portion of the site devoted to the nonconforming use is not expanded and the usage intensity (the number of people per acre) is not increased above the levels existing at the time of adoption of this *Compatibility Plan*. No ALUC review of such changes is required.
 - (c) ALUC review is required for any proposed expansion of a nonconforming use (in terms of the site size or the number of dwelling units or people on the site). Factors to be considered in such reviews include whether the development qualifies as infill (Policy 3.3.1) or warrants approval because of other special conditions (Policy 3.3.6).

Table 5: Basic Compatibility Criteria - 2004 Compatibility Plan (Table 2A)

Zone	Locations	Maximum Densities / Intensities				Req'd Open Land ³	Additional Criteria	
		Residential (d.u./ac) ¹	Other Uses (people/ac) ²				Prohibited Uses ⁴	Other Development Conditions ⁵
		Average ⁶	Single Acre ⁷	with Bonus ⁸				
A	Runway Protection Zone and within Building Restriction Line	0	0	0	0	All Remaining	<ul style="list-style-type: none"> > All structures except ones with location set by aeronautical function > Assemblages of people > Objects exceeding FAR Part 77 height limits > Storage of hazardous materials > Hazards to flight ⁹ 	<ul style="list-style-type: none"> > Avigation easement dedication
B1	Inner Approach/Departure Zone	0.05 (average parcel size ≥20.0 ac.)	25	50	65	30%	<ul style="list-style-type: none"> > Children's schools, day care centers, libraries > Hospitals, nursing homes > Places of worship > Bldgs with >2 aboveground habitable floors > Highly noise-sensitive outdoor nonresidential uses ¹⁰ > Aboveground bulk storage of hazardous materials ¹¹ > Critical community infrastructure facilities ¹² > Hazards to flight ⁹ 	<ul style="list-style-type: none"> > Locate structures maximum distance from extended runway centerline > Minimum NLR of 25 dB in residences (including mobile homes) and office buildings ¹³ > Airspace review required for objects >35 feet tall ¹⁴ > Avigation easement dedication
B2	Adjacent to Runway	0.1 (average parcel size ≥10.0 ac.)	100	200	260	No Req't	Same as Zone B1	<ul style="list-style-type: none"> > Locate structures maximum distance from runway > Minimum NLR of 25 dB in residences (including mobile homes) and office buildings ¹³ > Airspace review required for objects >35 feet tall ¹⁴ > Avigation easement dedication
B3	Extended Approach/Departure Zone	0.2 (average parcel size ≥5.0 ac.)	75	150	195	20%	<ul style="list-style-type: none"> > Children's schools, day care centers, libraries > Hospitals, nursing homes > Bldgs with >3 aboveground habitable floors > Highly noise-sensitive outdoor nonresidential uses ¹⁰ > Hazards to flight ⁹ 	<ul style="list-style-type: none"> > Minimum NLR of 20 dB in residences (including mobile homes) and office buildings ¹³ > Airspace review required for objects >70 feet tall ¹⁵ > Deed notice required
D	Primary Traffic Patterns and Runway Buffer Area	(1) ≤0.2 (average parcel size ≥5.0 ac.) or ¹⁶ (2) ≥5.0 (average parcel size ≤0.2 ac.)	100	300	390	10%	<ul style="list-style-type: none"> > Highly noise-sensitive outdoor nonresidential uses ¹⁰ > Hazards to flight ⁹ 	<ul style="list-style-type: none"> > Airspace review required for objects >70 feet tall ¹⁵ > Children's schools, hospitals, nursing homes discouraged ¹⁷ > Deed notice required
E	Other Airport Environs	No Limit	No Limit ¹⁸		No Req't	Hazards to flight ⁹	<ul style="list-style-type: none"> > Airspace review required for objects >100 feet tall ¹⁵ > Major spectator-oriented sports stadiums, amphitheaters, concert halls discouraged beneath principal flight tracks ¹⁸ 	
*	Height Review Overlay	Same as Underlying Compatibility Zone			Not Applicable	Same as Underlying Compatibility Zone	<ul style="list-style-type: none"> > Airspace review required for objects >35 feet tall ¹⁴ > Avigation easement dedication 	

See Chapter 3 for airport-specific additions or exceptions to these policies.

Notes: Numbered notes referenced in this table refer to notes in Table 2A of the 2004 Compatibility Plan.

NOTICE OF PUBLIC HEARING

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Thursday, from 8:00 a.m. to 5:00 p.m., except Monday, September 7 (Labor Day).

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon St., 1st Floor Hearing Room
Riverside, California

DATE OF HEARING: September 10, 2015

TIME OF HEARING: 9:00 A.M.

CASE DESCRIPTION:

ZAP1059BD15 – Wagner Architecture Group, Inc., for GEOVEL, Inc. (Representative: Jessica Peat or Walt Wagner) – City of Indio Case Nos. CUP 15-6-009 (Conditional Use Permit) and DR 15-5-387 (Design Review). A proposal to establish an automobile dealership (Chrysler Dodge Jeep Ram) within a 4-acre portion of a 17.94-acre parcel located on the northerly side of Varner Road, easterly of its intersection with Adams Street. (The parcel - Assessor's Parcel Number 607-230-002 – extends northerly to 40th Avenue.) The applicant proposes a 41,413 square foot main building (36,921 square feet excluding canopy) and a 1,300 square foot car wash. An additional 12,115 square foot building and outdoor parking spaces are proposed on an adjacent 4.95 acres of the same parcel. (Airport Compatibility Zones B1, B2 and D of the Bermuda Dunes Airport Influence Area).

FURTHER INFORMATION: Contact Russell Brady at (951) 955-0549 or John Guerin at (951) 955-0982. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Mr. Juan Rodriguez of the City of Indio Planning Department, at (760) 391-4016.

APPLICATION FOR MAJOR LAND USE ACTION REVIEW
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

ALUC Identification No.

ZAP1059BD15

PROJECT PROPONENT (TO BE COMPLETED BY APPLICANT)

Date of Application 07.27.2015
 Property Owner GEOVEL, INC. (Attn: George Velarde) Phone Number ~~(866) 979-7236~~
 Mailing Address 79125 Highway 111 (760) 578-0356
La Quinta, CA 92253

Agent (if any) _____ Phone Number _____
 Mailing Address _____

PROJECT LOCATION (TO BE COMPLETED BY APPLICANT)

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address 79200 Varner Road, Indio, CA
 Assessor's Parcel No. 607-230-002 Parcel Size 4 acres / 4.95 acres
 Subdivision Name _____ Zoning Classification BP
 Lot Number _____

PROJECT DESCRIPTION (TO BE COMPLETED BY APPLICANT)

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe) VACANT LOT

Proposed Land Use (describe) AUTOMOTIVE DEALERSHIP & PARKING FOR SAME

For Residential Uses Number of Parcels or Units on Site (exclude secondary units) _____
 For Other Land Uses Hours of Use 9AM - 9PM
 (See Appendix C) Number of People on Site Maximum Number 138
 Method of Calculation CBC MAX. OCCUPANCY x 50%
 (See Site Plan Building Information Table for detailed occupancy info.)

Height Data Height above Ground or Tallest Object (including antennas and trees) _____ 30' ft.
 Highest Elevation (above sea level) of Any Object or Terrain on Site _____ 107.2' ft.

Flight Hazards Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight? Yes No
 If yes, describe _____

S. Owned
 1, B2, D

REFERRING AGENCY (APPLICANT OR JURISDICTION TO COMPLETE)	
Date Received	
Agency Name	City of Indio
Staff Contact	Juan Rodriguez
Phone Number	(760) 391-4816
Agency's Project No.	CUP 15-6-1009 / ORIS-5-387
Type of Project	<input type="checkbox"/> General Plan Amendment <input type="checkbox"/> Zoning Amendment or Variance <input type="checkbox"/> Subdivision Approval <input type="checkbox"/> Use Permit <input type="checkbox"/> Public Facility <input type="checkbox"/> Other

A. NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. SUBMISSION PACKAGE:

ALUC REVIEW

- 1. Completed Application Form
- 1. Project Site Plan – Folded (8-1/2 x 14 max.)
- 1. Elevations of Buildings - Folded
- 1 Each . 8 ½ x 11 reduced copy of the above
- 1. 8 ½ x 11 reduced copy showing project in relationship to airport.
- 1 Set . Floor plans for non-residential projects
- 4 Sets. . Gummed address labels of the Owner and representative (**See Proponent**).
- 1 Set. . Gummed address labels of all property owners within a 300' radius of the project site. If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10), with ALUC return address.
- 4 Sets. . Gummed address labels of the referring agency (City or County).
- 1. Check for Fee (See Item "C" below)

STAFF REVIEW (Consult with ALUC staff planner as to whether project qualifies)

- 1. Completed Application Form
- 1. Project Site Plans – Folded (8-1/2 x 14 max.)
- 1. Elevations of Buildings - Folded
- 1. 8 ½ x 11 Vicinity Map
- 1 Set . Gummed address labels of the Owner and representative (**See Proponent**).
- 1 Set . Gummed address labels of the referring agency.
- 1. Check for review—See Below

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

STAFF REPORT

ADMINISTRATIVE ITEMS

- 4.1** Director's Approvals. As authorized pursuant to ALUC Resolution No. 2015-01, ALUC Director Ed Cooper has reviewed (via ZAP1140MA15) County of Riverside Change of Zone Case No. 7843, a proposal to change the zoning of 74.81 acres located easterly of Washington Street and northerly of Nandina Avenue in the unincorporated community of Woodcrest from R-A-1 (Residential Agricultural, one acre minimum lot size) to R-A-1 and R-A-10 (Residential Agricultural, 10 acre minimum lot size). This change of zone is being considered by the County in conjunction with Tentative Tract Map No. 36639, a proposal to divide the property into 52 single-family residential lots with a minimum gross lot size of one acre. Approximately 21.38 acres of the property would be changed from R-A-1 to R-A-10 zoning, with the remainder of the property remaining in the R-A-1 zone. The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (MARB/IP AIA), where residential density is not restricted, and is located more than 20,000 feet from the runway at March Air Reserve Base/Inland Port Airport at an elevation less than 200 feet above the runway elevation. ALUC Director Ed Cooper issued a determination of consistency for this Change of Zone on August 14, 2015.

Additionally, as authorized pursuant to Section 1.5.2(d) of the 2004 Riverside County Airport Land Use Compatibility Plan, ALUC Director Ed Cooper reviewed three non-legislative, non-residential cases (one each in the French Valley, Riverside Municipal, and MARB/IP Airport Influence Areas) and issued determinations of consistency.

ZAP1069RI15 (Zone D, Riverside Municipal AIA) pertains to a Conditional Use Permit application with the City of Riverside to establish an unmanned communications facility consisting of antennas on a 45-foot high monopine (with associated equipment cabinets and emergency generator) on a 430.22 square foot lease area within a 0.36-acre property located on the southerly side of Jurupa Avenue, opposite from its intersection with Columbus Avenue. Given the site's proximity to the runway (2,800 feet) and relative elevation, the applicant filed Form 7460-1 with the Federal Aviation Administration Obstruction Evaluation Service and obtained a "Determination of No Hazard to Air Navigation."

ZAP1065FV15 (Zone E, French Valley AIA) pertains to a Conditional Use Permit application with the City of Murrieta to establish an unmanned telecommunications facility consisting of antennas on a 55-foot high mono-eucalyptus (with associated equipment enclosure and related facilities) on a 546 square foot lease area within a 4.58-acre property located on the northerly side of Crawford Canyon Road, westerly of its intersection with Jessie Ceas Lane. Given that the elevation above mean sea level would be more than 100 feet greater than the elevation of the runway at a distance of only 7,879 feet from the closest point on the runway, Form 7460-1 was filed with the Federal Aviation Administration (FAA) Obstruction Evaluation Service and a "Determination of No hazard to Air Navigation" was issued. The FAA Determination letter includes special noninterference conditions that were incorporated in the ALUC Director's consistency letter.

ZAP1139MA15 (Zone C2, March AIA) pertains to a Conditional Use Permit application with the City of Riverside to re-establish a convenience store and service station and retain an existing drive-thru on a 0.72-acre property located on the westerly side of Mission Grove Parkway South, southerly of its intersection with Alessandro Boulevard. This item was handled as a Director's review as authorized by the Commission at its August 13 meeting in response to an oral communication from Mr. Atman Kadakia of Greens Group. Airport Compatibility Zone C2 allows up to 200 people per acre. Thus, the site could potentially be allowed to accommodate 144 persons. Staff determined that actual occupancy would be less than 100 persons. The proposed use would occupy existing structures; therefore, FAA height/elevation review is not applicable in this situation.

Copies of these consistency letters and background documents are attached, for the Commission's information.

4.2 Video/Live Streaming of Meetings Held in the County Administrative Center Board Room.

The Commission and staff have often heard complaints from members of the public relating to the fact that ALUC meetings occur during the workday, making it difficult for constituents to attend. In an effort to increase access to public meetings of government agencies and commissions, improve transparency, and make it easier for residents of Riverside County to be aware of government decisions and discussions, the Board of Supervisors has invested in the necessary audio-visual equipment to make its meetings and workshops held in the County Administrative Center (CAC) Board Room available to the public online via "live streaming," and keeping archived videos of those meetings available on the county's website.

The Board of Supervisors recently adopted a motion to direct the Executive Office, working with the Clerk of the Board, to develop procedures requiring that, within six months of passage of the proposal, organizations subject to the Brown Act that include one or more County appointees wishing to use the CAC Board Room make their meetings (to the extent that they are held therein) available online to the public as part of their Room use agreement. Any costs for "streaming access to the network" would be borne by the County as part of the operating costs of the CAC Board Room. The only start-up cost would be modifications to ALUC's webpage to create a link to the meeting videos. Meetings held at other locations, such as our upcoming December meeting in Cathedral City, would not be required to be available via "live streaming."

Staff would support making ALUC meetings held in the CAC Board Room available to the public in this manner – especially since the Commissioners have always been quite dapper!

Y:\ALUC\ALUC Administrative Items\Admin. 2015\Admin Item 09-10-15.doc

**SUBMITTAL TO THE BOARD OF SUPERVISORS
COUNTY OF RIVERSIDE, STATE OF CALIFORNIA**



FROM: Supervisor Kevin Jeffries

SUBMITTAL DATE:
July 15, 2015

SUBJECT: Requiring Video Access to Government Meetings Held in the CAC Board Room]

RECOMMENDED MOTION: That the Board of Supervisors:
Direct the Executive Office to work with the Clerk of the Board to develop procedures requiring Brown Act organizations containing a Member or Members of the Board of Supervisors or their Appointees who wish to use the CAC Board Room to allow their meetings to be made available to the public online.

BACKGROUND:

Summary

Public access to meetings of government bodies is required by State Law and is critical to having an informed electorate. Unfortunately, many of those meetings take place during the workday when it is difficult for residents to attend. The Board of Supervisors has tried to mitigate that difficulty by making its own Board meetings and workshops available to the public via live streaming online, as well as keeping archived videos of meetings available on the county's website.

Background Continued on Second Page

KEVIN JEFFRIES
Supervisor, 1st District

FINANCIAL DATA	Current Fiscal Year:	Next Fiscal Year:	Total Cost:	Ongoing Cost:	POLICY/CONSENT (per Exec. Office)
COST	\$ Click here to	\$ 0	\$ Click here to	\$ 0	
NET COUNTY COST	\$ 0	\$ 0	\$ 0	\$ 0	Consent <input type="checkbox"/> Policy <input checked="" type="checkbox"/>

SOURCE OF FUNDS:	Budget Adjustment: n/a
	For Fiscal Year:

C.E.O. RECOMMENDATION:

County Executive Office Signature

MINUTES OF THE BOARD OF SUPERVISORS

On motion of Supervisor Jeffries, seconded by Supervisor Ashley and duly carried by unanimous vote, IT WAS ORDERED that the above matter is approved as recommended.

Ayes: Jeffries, Tavaglione, Washington, Benoit and Ashley
Nays: None
Absent: None
Date: July 21, 2015
xc: Supvr. Jeffries, E.O., COB

Kecia Harper-Ihem
Clerk of the Board
By: Deputy

Prev. Agn. Ref.: | **District:** | **Agenda Number:**

3-1

Departmental Concurrence

- A-30
- Positions Added
- 4/5 Vote
- Change Order

SUBMITTAL TO THE BOARD OF SUPERVISORS, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA

FORM 11: Requiring Video Access to Government Meetings Held in the CAC Board Room

DATE: July 15, 2015

PAGE: Page 2 of 2

BACKGROUND Continued:

The current county practice does not extend to every governmental agency/commission meeting in which Board Members or their appointees participate (e.g. WRCOG, RCTC, RCA, Planning Commission, etc), and many of those groups do not make their meetings publicly available on line. The Board of Supervisors has already invested in the necessary audio-visual equipment in the CAC Board Room to allow any group who utilizes those chambers to live stream their meetings without any additional staffing or hardware needs.

This proposal would require that within six months of passage of this proposal, any governmental agency or commission which is 1) Bound by the Brown Act, 2) Consists in whole or in part of members of the Board of Supervisors or their appointees, and 3) Wishes to use the CAC Board Room for their meetings, be requested to allow county staff make their meetings available online to the public as part of their agreement to use the chambers.

Because the equipment, staffing, and technology exists already in the Board Room, the only start-up cost to these agencies of this new requirement to begin broadcasting their meetings would be to modify their webpages to create a link to the videos of meetings. Any costs for streaming access to the network would be borne by the County as part of the operations costs of the Board Room itself.

Impact on Private Sector and Business

Increased access to public meetings of government agencies and commissions will improve transparency and make it easier for residents of Riverside County to be aware of government decisions and discussions.

PAGE BREAK



**AIRPORT LAND USE COMMISSION
RIVERSIDE COUNTY**



CHAIR

Simon Housman
Rancho Mirage

August 14, 2015

VICE CHAIRMAN

Rod Ballance
Riverside

Mr. Peter Lange, Contract Planner
Riverside County Planning Department
4080 Lemon Street, Twelfth Floor
Riverside, CA 92501

COMMISSIONERS

Arthur Butler
Riverside

RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW

Glen Holmes
Hemet

File No.: ZAP1140MA15
Related File No.: CZ07843 (Change of Zone) [associated with TR36639
(Tentative Tract Map)]

John Lyon
Riverside

APN: 273-310-033; 273-310-034

Greg Pettis
Cathedral City

Dear Mr. Lange:

Steve Manos
Lake Elsinore

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to ALUC Resolution No. 2015-01 (as adopted on August 13, 2015), staff reviewed Change of Zone Case No. 7843 (CZ07843), a proposal to change the zoning of 74.81 acres located easterly of Washington Street, northerly of Nandina Avenue, and southerly of a straight-line easterly or westerly extension of Mariposa Avenue in the unincorporated community of Woodcrest from R-A-1 (Residential Agricultural, one acre minimum lot size) to R-A-1 and R-A-10 (Residential Agricultural, 10 acre minimum lot size). This change of zone is being considered in conjunction with Tentative Tract Map No. 36639, a proposal to divide the property into 52 single-family residential lots with a minimum gross lot size of one acre. Approximately 21.38 acres of the property would be changed from R-A-1 to R-A-10 zoning, with the remainder of the property remaining in the R-A-1 zone.

STAFF

Director
Ed Cooper

John Guerin
Russell Brady
Barbara Santos

County Administrative Center
4080 Lemon St, 14th Floor
Riverside, CA 92501
(951) 955-5132

The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E, residential density is not restricted.

www.rcaluc.org

The elevation of Runway 14-32 at its northerly terminus is approximately 1,535 feet above mean sea level (1535 feet AMSL). The proposed maximum pad elevation is 1,640 feet AMSL. The R-A zone allows a maximum building height of 40 feet for single-family residences, for a total maximum elevation of 1,680 feet AMSL. However, the site is located beyond the 20,000 foot radius from the runway at March Air Reserve Base/Inland Port Airport. Therefore, Federal Aviation Administration (FAA) obstruction evaluation review for height/elevation reasons would not be required.

As ALUC Director, I hereby find the above-referenced Change of Zone **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan.

This finding of consistency relates to airport compatibility issues and does not necessarily constitute an endorsement of the proposed zone change. As the site is located within Airport Compatibility Zone E, both the existing and the proposed zoning are consistent with the March ALUCP.

While the change of zone is not subject to conditions, we would further recommend that the following conditions be applied to the Tentative Tract Map:

CONDITIONS (recommended for the proposed Tentative Tract Map):

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the project and shall be prohibited at this site, in accordance with Note 1 on Table 4 of the Lake Mathews/Woodcrest Area Plan:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area.
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached notice shall be provided to all potential purchasers of the proposed lots and to tenants of the homes thereon.
4. Any new aboveground detention or bioretention basins on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention/retention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.

AIRPORT LAND USE COMMISSION

August 14, 2015

5. The following uses/activities are specifically prohibited: wastewater management facilities; trash transfer stations that are open on one or more sides; recycling centers containing putrescible wastes; incinerators.

If you have any questions, please contact John Guerin, Principal Planner, at (951) 955-0982.

Sincerely,

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Edward C. Cooper, Director

Attachments: Notice of Airport in Vicinity

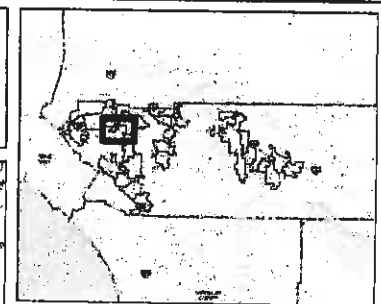
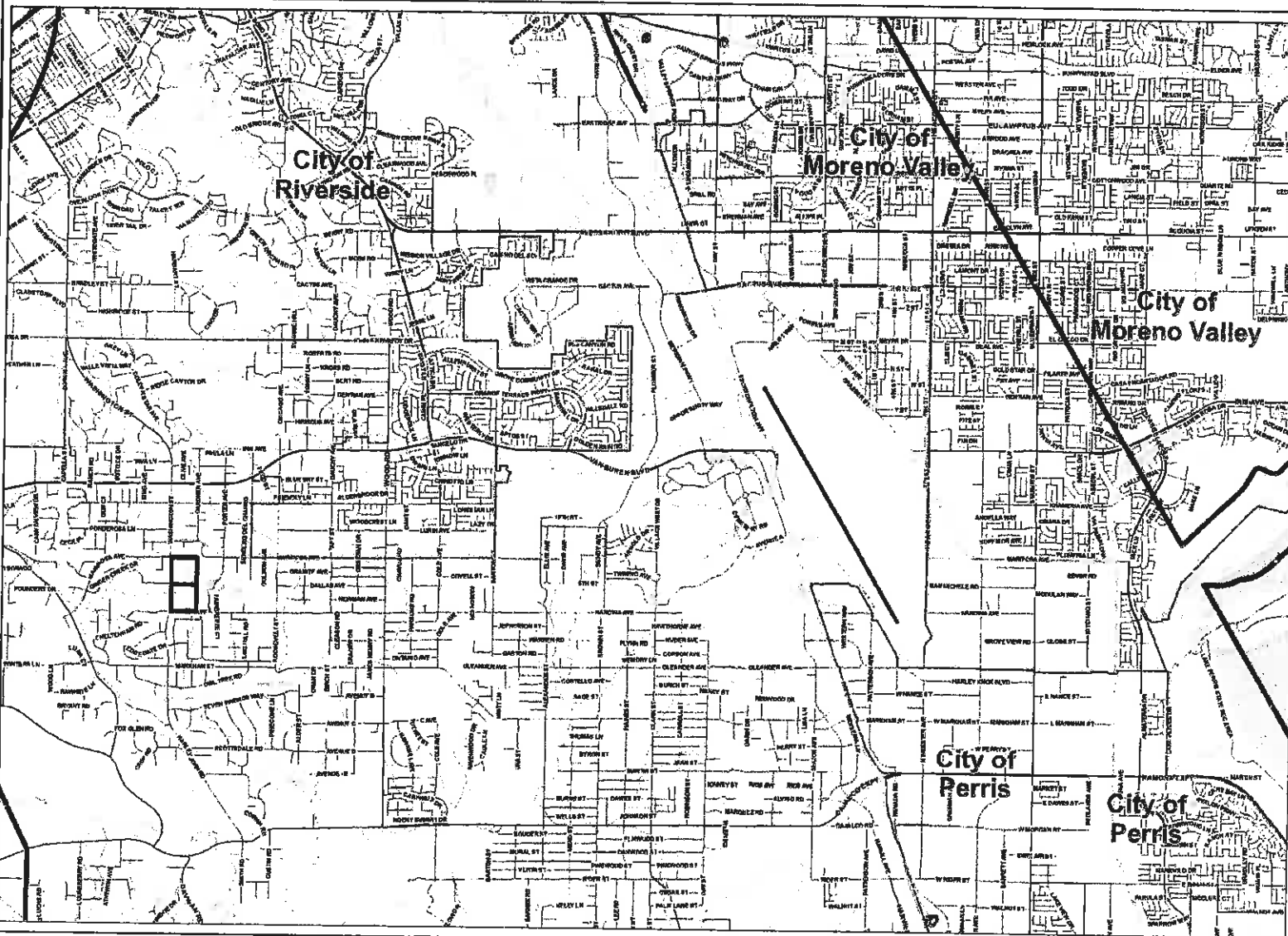
cc: Kevin and Pauline Doan (applicant/owner/payee)
Adkan Engineers – Attn.: Michael Brendecke (project representative)
Sexton Real Estate – Attn.: Leroy Nichols (project representative)
Charissa Leach, Adkan Engineers
Gary Gosliga, Airport Manager, March Inland Port Airport Authority
Denise Hauser or Sonia Pierce, March Air Reserve Base
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1140MA15\ZAP1140MA15.LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

My Map



Legend

- AIA
- Runways
- City Boundaries
- Faults**
 - <all other values>
 - ALQUIST-PRIOLO
 - RIVERSIDE COUNTY
- Fault Zones**
 - <all other values>
 - COUNTY FAULT ZONE
 - ELSINORE FAULT ZONE
 - SAN ANDREAS FAULT ZONE
 - SAN JACINTO FAULT ZONE
- adjacent_highways**
 - Interstate
 - Interstate 3
 - State Highways; 60
 - State Highways 3
 - US HWY
 - OUT
- highways_large**
 - HWY
 - INTERCHANGE
 - INTERSTATE
 - USHWY
- counties
- cities



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

0 9,097 18,194 Feet

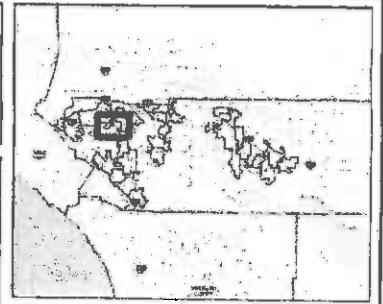
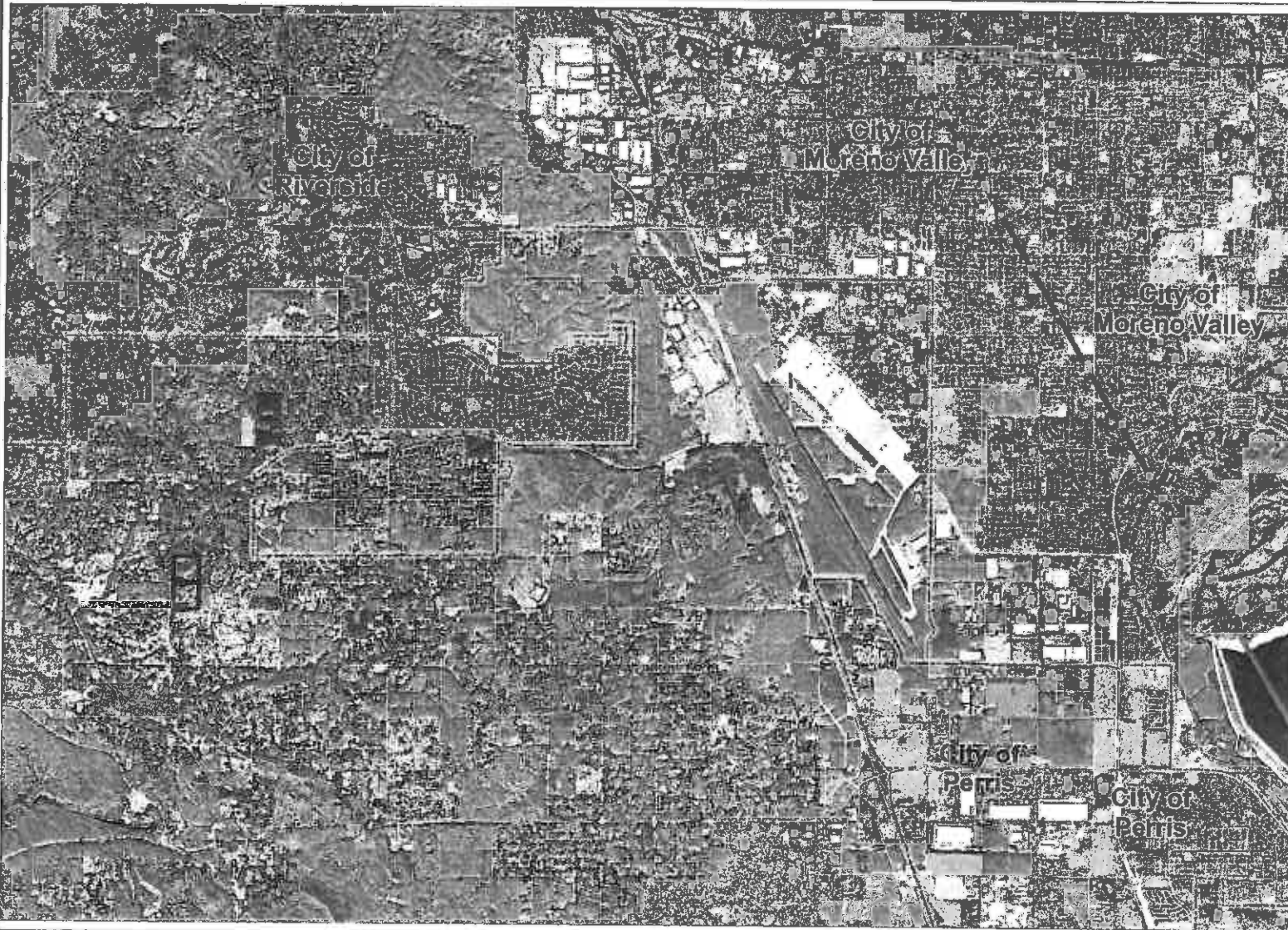


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Notes

My Map



Legend

- AIA
- Runways
- City Boundaries
- Faults**
 - <all other values>
 - ALQUIST-PRIOLO
 - RIVERSIDE COUNTY
- Fault Zones**
 - <all other values>
 - COUNTY FAULT ZONE
 - ELSINORE FAULT ZONE
 - SAN ANDREAS FAULT ZONE
 - SAN JACINTO FAULT ZONE



0

9,097

18,194 Feet



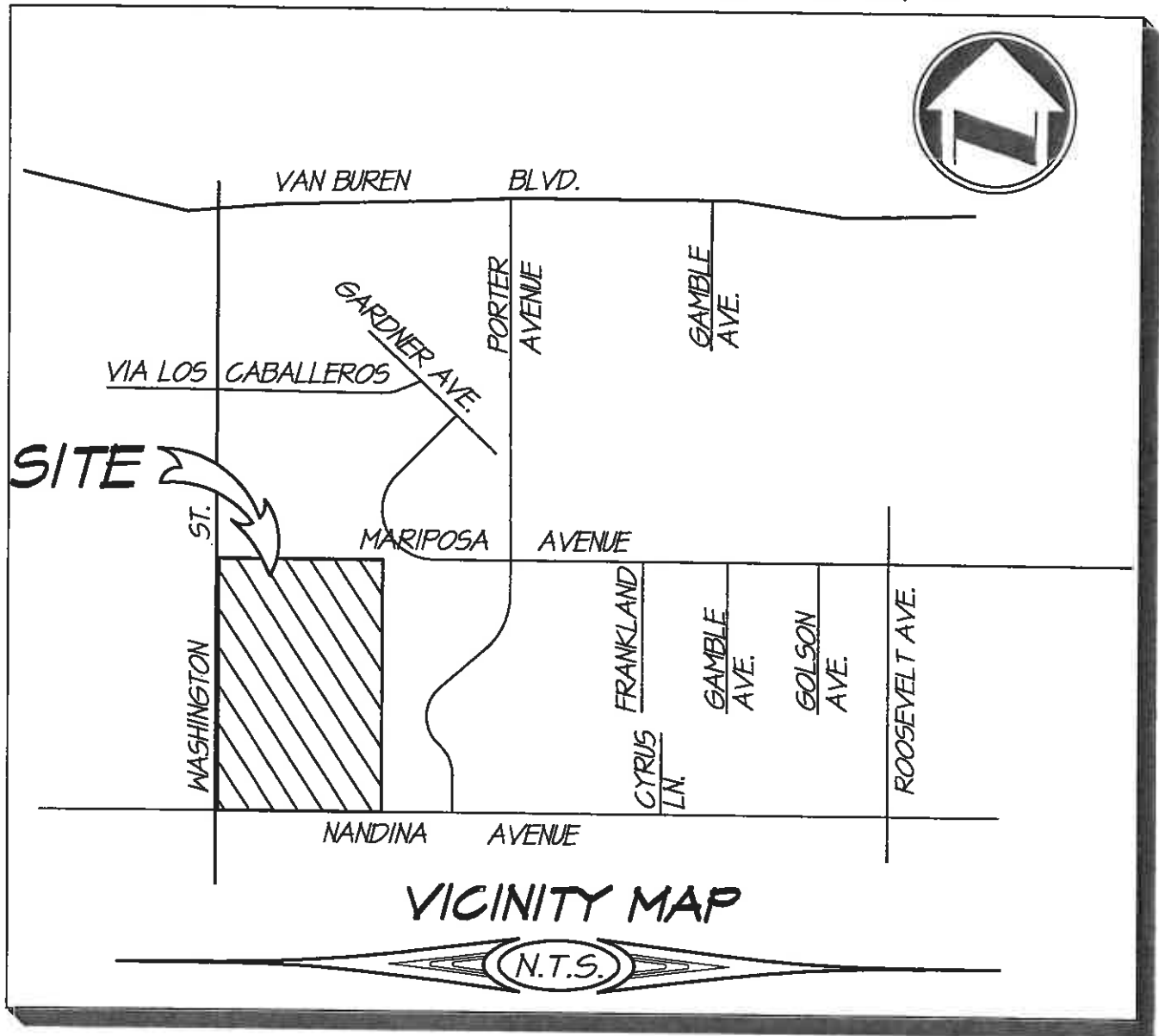
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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Notes

THOMAS BROS. GUIDE PAGE 746, GRID J5, J6



SEC.36 T.35. R.4W

IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA
CHANGE OF ZONE

LOT 5 OF WOODCREST ACRES NO. 4, IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS SHOWN BY MAP ON FILE IN BOOK 13 PAGE 24, RECORDS OF RIVERSIDE COUNTY, CALIFORNIA

OWNER / APPLICANT
 KEVIN T. & PAULA L. DODD
 405 LINDSEY MANUELS - SECTION 28 REAL ESTATE
 5105 ARLINGTON AVE, RIVERSIDE, CA 92506
 TEL: 951-500-1000

ENGINEER
ADKIN ENGINEERS
 1070 AVENUE ONE
 RIVERSIDE, CA 92504
 951-500-0244

ASSESSORS PARCEL NUMBERS
 273-290-038, 034

PROJECT DENSITY TABLE

UTILITY SURVEYORS
 WATER: MHDH
 SEWER: INDIVIDUAL SEPTIC SYSTEM
 GAS: SOUTHERN CALIFORNIA GAS COMPANY
 ELECTRICITY: SOUTHERN CALIFORNIA Edison
 TELEPHONE: SBC
 CABLE: RIVERSIDE UNIFIED
 CATH: QUARTER CABLE

ZONING/LAND USE

EXISTING ZONING: R-A-1
 PROPOSED ZONING: R-A-1R-A-10
 EXISTING LANDUSE: RC-VLDR, 1 RH

PROJECT NOTES

1. THOMAS BRDG COORDINATES 2008 RIV. CO. PHASE 1A GRID A-10
2. FLOOD ZONE C, NOT IN SPECIAL FLOOD AREA. SAID PROPERTY IS NOT SUBJECT TO OVERFLOW INUNDATION FLOOD HAZARD.
3. TOTAL NUMBER OF LOTS: 32
4. TOTAL GROSS ACRES: 14.81 ACRES
5. THE TENTATIVE MAP INCLUDES THE ENTIRE CONTIGUOUS OWNERSHIP OF THE LAND OWNERS.
6. LOT DIMENSIONS SHOWN HEREON INCLUDE DIMENSIONS 1 STREET R/W.
7. PROPERTY DOES NOT LIE WITHIN A COUNTY SERVICE A.
8. ALL LOTS ARE 1 ACRE MINIMUM BRDG SQUARE FOOTAGE.
9. ALL LOTS USE ADVANCED TREATMENT DRIP FIELDS FOR SEPTIC SYSTEMS.

LEGEND

Ⓢ DRAINAGE

LEGAL DESCRIPTION

PARCEL 1:
 THE NORTHWEST QUARTER OF THE NORTHWEST QUARTER OF SECTION 28, TOWNSHIP 3 SOUTH, RANGE 5 WEST, SAN BERNARDINO COUNTY AND MERRIDIAN IN THE COUNTY OF SAN BERNARDINO, STATE OF CALIFORNIA, AS SHOWN BY SECTIONIZED SURVEY OF D. SORRANTE DE SAN ANTONIO ON FILE IN BOOK 1 PAGE 6 OF MAPS, RECORDS OF SAN BERNARDINO COUNTY, CALIFORNIA.

EXCEPTING THEREFROM THE NORTHERLY 30 FEET AS CONVEYED TO CHARD, INCORPORATED, A CALIFORNIA CORPORATION, BY THAT DEED RECORDED NOVEMBER 25, 1989 AS INSTRUMENT NO. 12086 OF OFFICIAL RECORDS.

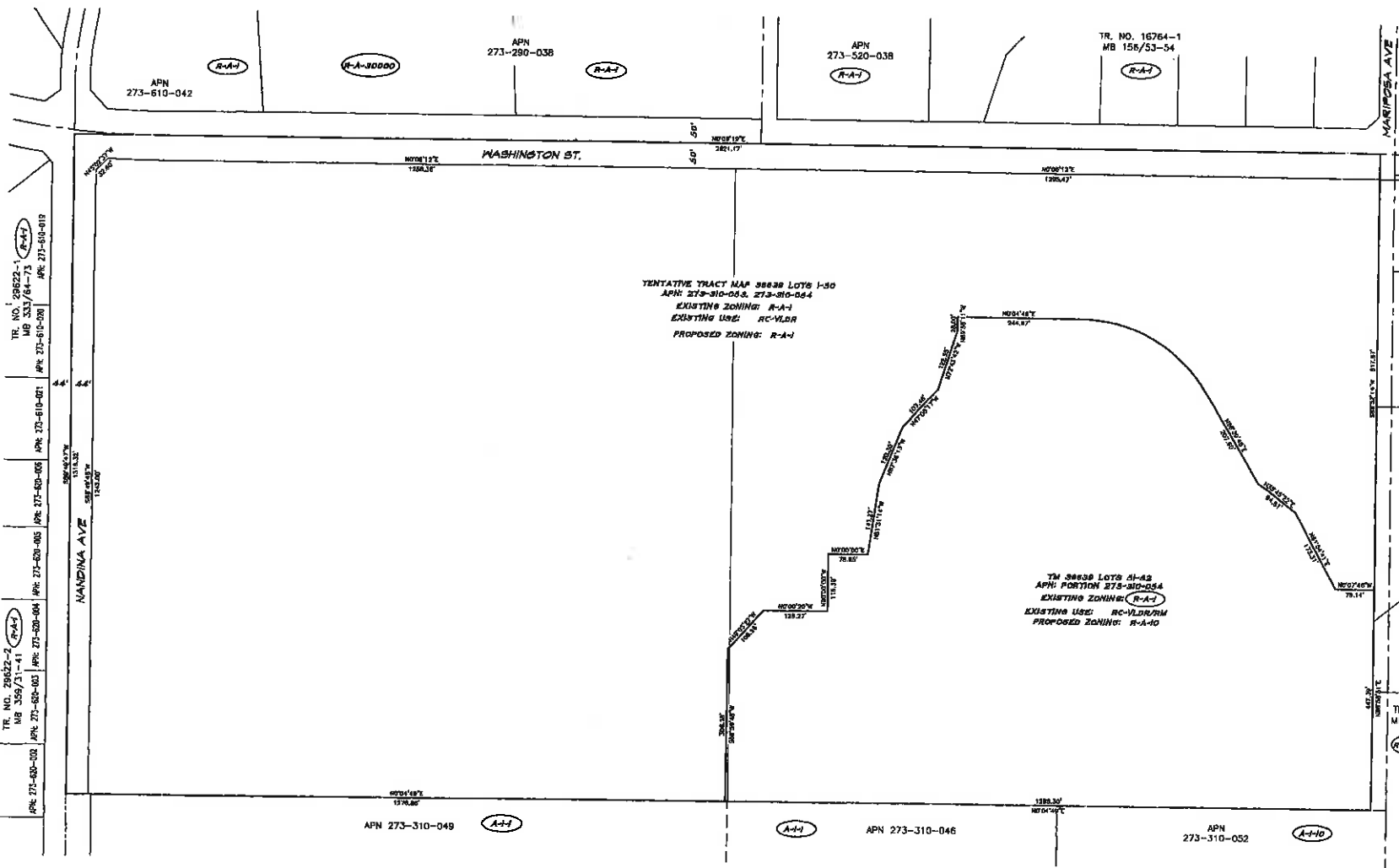
ALSO EXCEPTING THEREFROM AN INDIVIDUAL ONE-HALF INTEREST IN ALL MINERALS, OIL, GAS AND OTHER HYDROCARBON SUBSTANCES 1/2000 PARTIAL UNDER OR BENEFIT OR WHICH MAY BE PRODUCED OR SAVED FROM SAID LAND, AS RESERVED BY JERRY D. BRIDGES AND OTHER, HUSBAND AND WIFE, BY THAT DEED RECORDED JULY 12, 1981 IN BOOK 1206 PAGE 216 OF OFFICIAL RECORDS.

PARCEL 2:
 THE SOUTHWEST QUARTER OF THE NORTHWEST QUARTER OF SECTION 28, TOWNSHIP 3 SOUTH, RANGE 5 WEST, SAN BERNARDINO COUNTY AND MERRIDIAN IN THE COUNTY OF SAN BERNARDINO, STATE OF CALIFORNIA, AS SHOWN BY SECTIONIZED SURVEY OF D. SORRANTE DE SAN ANTONIO ON FILE IN BOOK 1 PAGE 6 OF MAPS, RECORDS OF SAN BERNARDINO COUNTY, CALIFORNIA.

ALSO EXCEPTING THEREFROM AN INDIVIDUAL ONE-HALF INTEREST IN ALL MINERALS, OIL, GAS AND OTHER HYDROCARBON SUBSTANCES 1/2000 PARTIAL UNDER OR BENEFIT OR WHICH MAY BE PRODUCED OR SAVED FROM SAID LAND, AS RESERVED BY JERRY D. BRIDGES AND OTHER, HUSBAND AND WIFE, BY THAT DEED RECORDED JULY 12, 1981, IN BOOK 1206 PAGE 216 OF OFFICIAL RECORDS.

REVISIONS		
NO.	DESCRIPTION	DATE

CHANGE OF ZONE
 PREPARATION DATE: 1.11.2014
 DRAWN BY:
adkin ENGINEERS
 1070 AVENUE ONE, RIVERSIDE, CA 92504
 TEL: (951) 500-0244 FAX: (951) 500-0244



TR. NO. 29622-1
 MB 333/31-41
 APR. 27-610-010

TR. NO. 29622-2
 MB 359/31-41
 APR. 27-610-011

TR. NO. 29622-3
 MB 359/31-41
 APR. 27-610-012

TR. NO. 29622-4
 MB 359/31-41
 APR. 27-610-013

TR. NO. 29622-5
 MB 359/31-41
 APR. 27-610-014

TR. NO. 29622-6
 MB 359/31-41
 APR. 27-610-015

TR. NO. 29622-7
 MB 359/31-41
 APR. 27-610-016

TR. NO. 29622-8
 MB 359/31-41
 APR. 27-610-017

TR. NO. 29622-9
 MB 359/31-41
 APR. 27-610-018

TR. NO. 29622-10
 MB 359/31-41
 APR. 27-610-019

TR. NO. 29622-11
 MB 359/31-41
 APR. 27-610-020

TR. NO. 29622-12
 MB 359/31-41
 APR. 27-610-021

TR. NO. 29622-13
 MB 359/31-41
 APR. 27-610-022

TR. NO. 29622-14
 MB 359/31-41
 APR. 27-610-023

TR. NO. 29622-15
 MB 359/31-41
 APR. 27-610-024

TR. NO. 29622-16
 MB 359/31-41
 APR. 27-610-025

TR. NO. 29622-17
 MB 359/31-41
 APR. 27-610-026

TR. NO. 29622-18
 MB 359/31-41
 APR. 27-610-027

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 MB 359/31-41
 APR. 27-610-028

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 MB 359/31-41
 APR. 27-610-029

TR. NO. 29622-21
 MB 359/31-41
 APR. 27-610-030

TR. NO. 29622-22
 MB 359/31-41
 APR. 27-610-031

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 APR. 27-610-032

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 APR. 27-610-040

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 APR. 27-610-043

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 APR. 27-610-049

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 APR. 27-610-050

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 APR. 27-610-051

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 MB 359/31-41
 APR. 27-610-052

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 APR. 27-610-054

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 APR. 27-610-058

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 MB 359/31-41
 APR. 27-610-059

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 MB 359/31-41
 APR. 27-610-060

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 MB 359/31-41
 APR. 27-610-063

TR. NO. 29622-55
 MB 359/31-41
 APR. 27-610-064

TR. NO. 29622-56
 MB 359/31-41
 APR. 27-610-065

TR. NO. 29622-57
 MB 359/31-41
 APR. 27-610-066

TR. NO. 29622-58
 MB 359/31-41
 APR. 27-610-067

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 MB 359/31-41
 APR. 27-610-068

TR. NO. 29622-60
 MB 359/31-41
 APR. 27-610-069

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 MB 359/31-41
 APR. 27-610-070

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 MB 359/31-41
 APR. 27-610-071

TR. NO. 29622-63
 MB 359/31-41
 APR. 27-610-072

TR. NO. 29622-64
 MB 359/31-41
 APR. 27-610-073

TR. NO. 29622-65
 MB 359/31-41
 APR. 27-610-074

TR. NO. 29622-66
 MB 359/31-41
 APR. 27-610-075

TR. NO. 29622-67
 MB 359/31-41
 APR. 27-610-076

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 MB 359/31-41
 APR. 27-610-077

TR. NO. 29622-69
 MB 359/31-41
 APR. 27-610-078

TR. NO. 29622-70
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 APR. 27-610-082

TR. NO. 29622-74
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 APR. 27-610-083

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 MB 359/31-41
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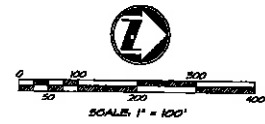
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 APR. 27-610-098

TR. NO. 29622-90
 MB 359/31-41
 APR. 27-610-099

TR. NO. 29622-91
 MB 359/31-41
 APR. 27-610-100



PAGE BREAK



AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY



August 5, 2015

CHAIR

Simon Housman
Rancho Mirage

Ms. Gaby Adame, Assistant Planner

VICE CHAIRMAN

Rod Ballance
Riverside

City of Riverside Community Development Department/Planning Division
3900 Main Street, Third Floor
Riverside, CA 92522

COMMISSIONERS

Arthur Butler
Riverside

Glen Holmes
Hemet

John Lyon
Riverside

RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW

File No.: ZAP1069RI15
Related File No.: CUP 15-0150
APN: 189-072-002

Dear Ms. Adame:

Greg Pettis
Cathedral City

Steve Manos
Lake Elsinore

Under the delegation of the Riverside County Airport Land Use Commission (ALUC), staff reviewed the above-referenced proposal for the establishment of an unmanned telecommunications facility consisting of antennas on a 45-foot high monopine, with associated equipment cabinets and emergency generator, on a 430.22 square foot lease area within a 0.36-acre property located on the southerly side of Jurupa Avenue, opposite from its intersection with Columbus Avenue, in the City of Riverside.

STAFF

Director
Ed Cooper

John Guerin
Russell Brady
Barbara Santos

The site is located within Airport Compatibility Zone D of the Riverside Municipal Airport Influence Area (AIA). Given the site's proximity to the runway (approximately 2,800 feet) and relative elevation, the applicant filed Form 7460-1 with the Federal Aviation Administration Obstruction Evaluation Service and obtained a "Determination of No Hazard to Air Navigation."

County Administrative Center
4080 Lemon St., 14th Floor.
Riverside, CA 92501
(951) 955-5132

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2005 Riverside Municipal Airport Land Use Compatibility Plan, subject to the following conditions:

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky.
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.

www.rcaluc.org

- (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area, including but not limited to, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, artificial marshes, incinerators, and fly ash disposal.
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached notice shall be provided to all potential purchasers and lessees of the property and tenants of any habitable buildings thereon, and shall be recorded as a deed notice.
 4. The Federal Aviation Administration has conducted an aeronautical study of the proposed structure (Aeronautical Study No. 2015-AWP-4533-OE) and has determined that neither marking nor lighting of the structure is necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 K Change 2 and shall be maintained in accordance therewith for the life of the project.
 5. The maximum height of the proposed structure shall not exceed 45 feet above ground level, and the maximum elevation of the proposed structure, including all towers and antennas, shall not exceed 873 feet above mean sea level.
 6. The specific coordinates, height, top point elevation of the proposed structure, frequencies, and power shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in building height or elevation shall not require further review by the Airport Land Use Commission.
 7. Temporary construction equipment used during actual construction of the structure shall not exceed the height of the structure, unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.
 8. Within five (5) days after construction of the structure reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned.

AIRPORT LAND USE COMMISSION

August 5, 2015

If you have any questions, please contact Russell Brady, Contract Planner, at (951) 955-0549, or John Guerin, Principal Planner, at (951) 955-0982.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Edward C. Cooper, Director

RB:bks

Attachments: Notice of Airport in Vicinity
FAA Aeronautical Study No. 2015-AWP-4533-OE

cc: Core Development Services – Attn.: Henry Castro (applicant/representative)
Duman, c/o Secil and John Duman (property owner)
Kim Ellis, Manager, Riverside Municipal Airport
ALUC Case File

Y:\AIRPORT CASE FILES\Riverside\ZAP1069RI15\ZAP1069RI15LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 2601 Meacham Boulevard
 Fort Worth, TX 76193

Aeronautical Study No.
 2015-AWP-4533-OE

Issued Date: 06/04/2015

Henry Castro
 Verizon-Republic
 2749 Saturn St
 Brea, CA 92821

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Monopole Verizon-Republic
 Location: Jurupa Valley, CA
 Latitude: 33-57-37.58N NAD 83
 Longitude: 117-26-26.42W
 Heights: 828 feet site elevation (SE)
 45 feet above ground level (AGL)
 873 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed and maintained in accordance with FAA Advisory circular 70/7460-1 K Change 2.

This determination expires on 12/04/2016 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates , heights, frequency(ies) and power . Any changes in coordinates , heights, and frequencies or use of greater power will void this determination. Any future construction or alteration , including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

A copy of this determination will be forwarded to the Federal Communications Commission (FCC) because the structure is subject to their licensing authority.

If we can be of further assistance, please contact our office at (425) 227-2625. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2015-AWP-4533-OE.

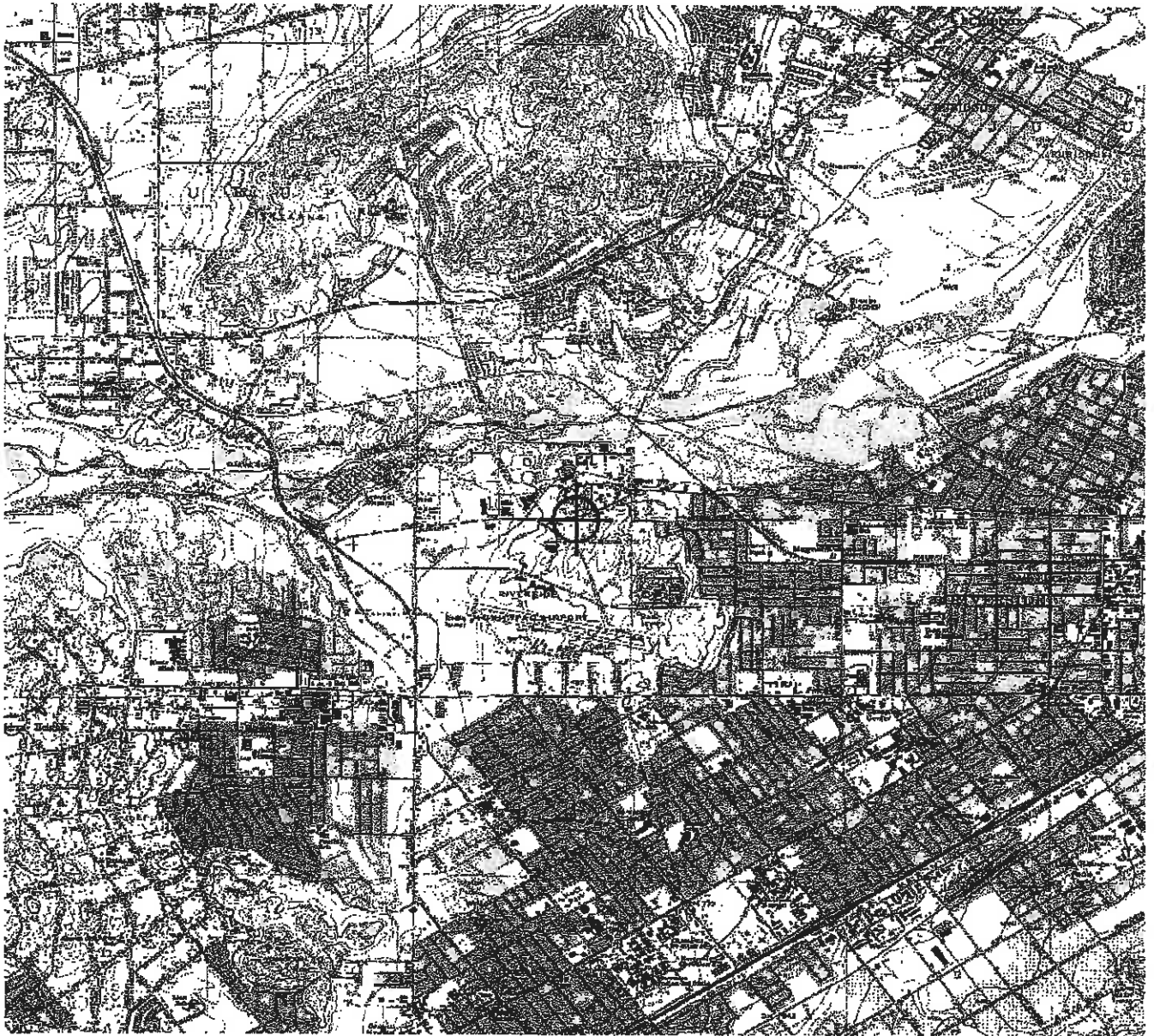
Signature Control No: 249101185-254019999
Paul Holmquist
Technician

(DNE)

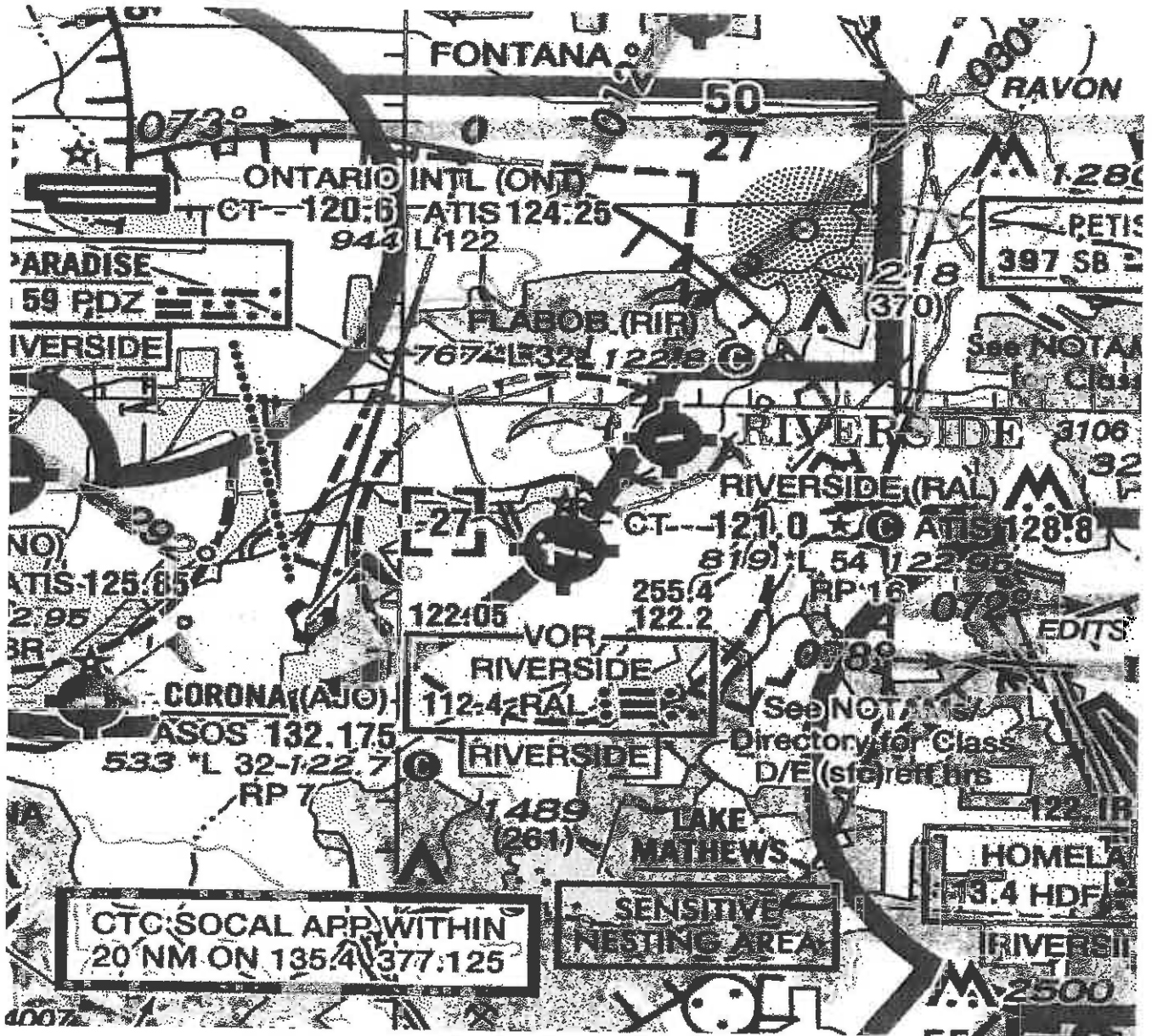
Attachment(s)
Map(s)

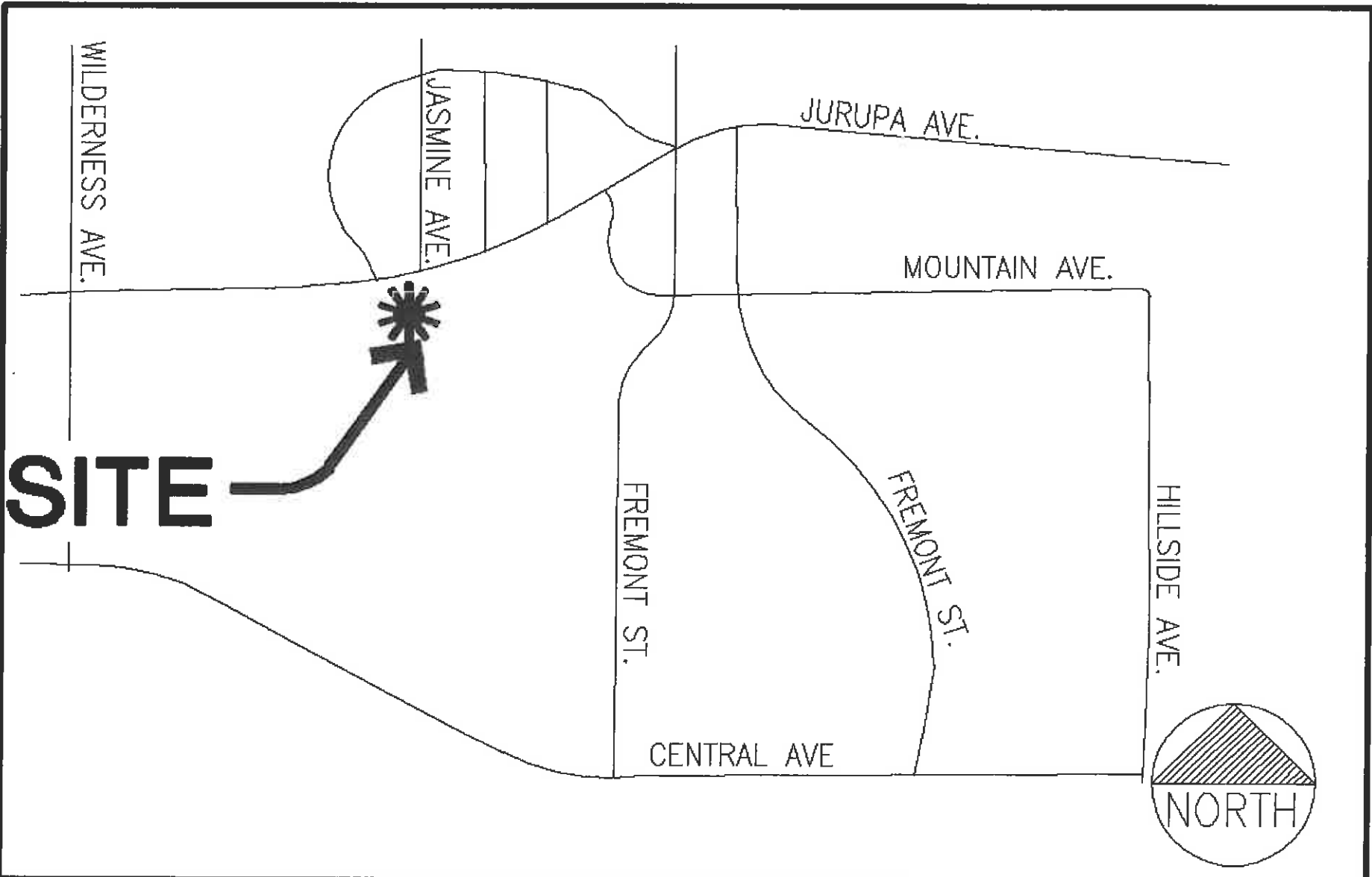
cc: FCC

TOPO Map for ASN 2015-AWP-4533-OE



Sectional Map for ASN 2015-AWP-4533-OE

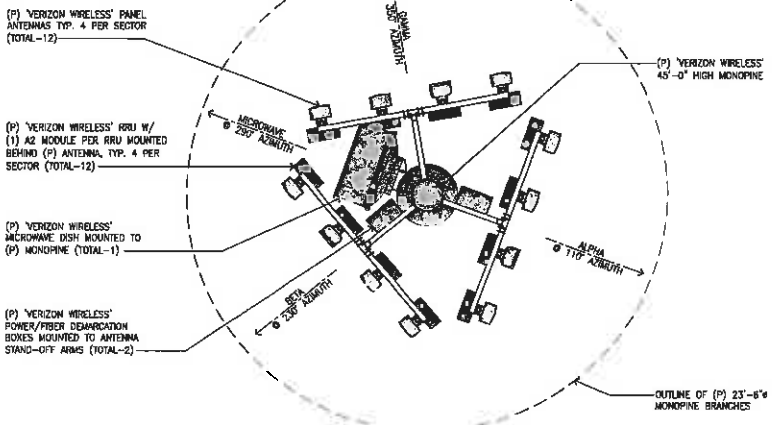




SITE

VICINITY MAP

NOTES:
 1. ALL (P) ANTENNAS AND EQUIPMENT MOUNTED TO THE (P) POLE SHALL BE PAINTED TO MATCH THE (P) MONOPINE. ALL ANTENNAS AND EQUIPMENT ON THE POLE SHALL BE COVERED WITH PINE SOCKS.
 2. (P) MONOPINE BRANCHES NOT SHOWN FOR CLARITY.



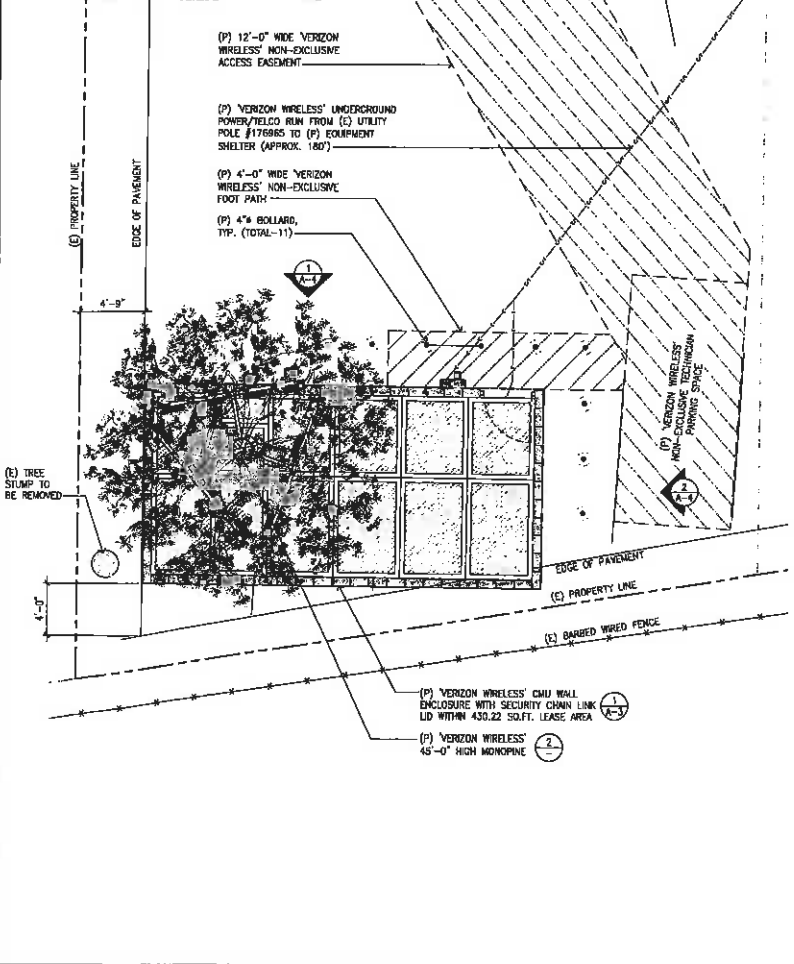
ANTENNA PLAN

SCALE 3/8"=1'-0" 0 1' 2' 4'

NOT USED

CABLE/ANTENNA SCHEDULE				
ANTENNA SECTOR	AZMUTH	ANTENNA MAKE/MODEL	CABLE LENGTH	CABLE SIZE
ALPHA	110°	TBD	60'	7/8"
BETA	230°	TBD	60'	7/8"
GAMMA	350°	TBD	60'	7/8"
MW	280°	TBD	50'	7/8"
(2) GPS	N/A	TBD	15'	1/2"

NOTE: CONSTRUCTION MANAGER TO FIELD VERIFY CABLE LENGTHS PRIOR TO ORDERING, FABRICATION OR INSTALLATION OF CABLES. CHECK RF DATA SHEET.



3 ENLARGED SITE PLAN

SCALE 3/8"=1'-0" 0 1' 2' 4'

REV	DATE/BY	DESCRIPTION
0	07/17/14	ISSUED FOR PER ZONING
1	07/29/14	ISSUED FOR 100% ZONING

ENGINEER / CONSULTANT

SITE BUILDER

15005 SAND CANYON AVENUE
 SUITE 10, FRESNO, CA 93720
 (714) 723-8484 (714) 352-4411 fax
 www.verizon.com

core
 A&E SERVICES
 2748 Salinas Street
 Fresno, California 93721
 (714) 723-8484 (714) 352-4411 fax
 www.core.ae.com

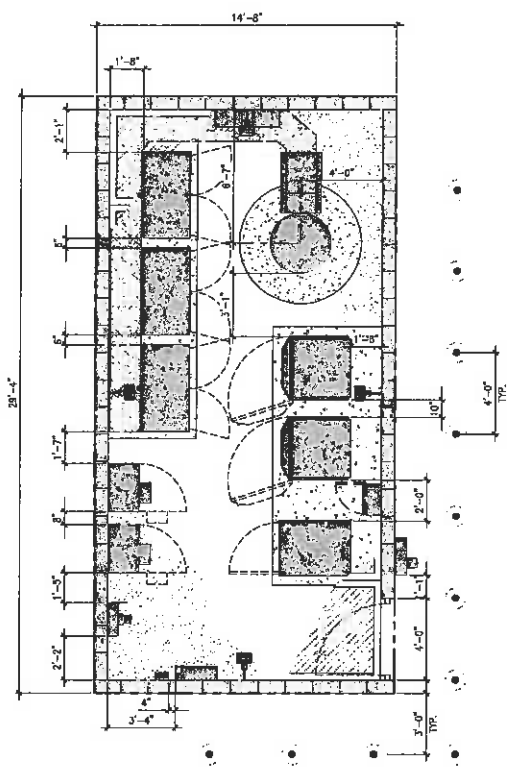
core
 DEVELOPMENT SERVICES
 2748 Salinas Street
 Fresno, California 93721
 (714) 723-8484 (714) 352-4411 fax
 www.core.ae.com

SITE NAME
REPUBLIC
 SITE ADDRESS
 6600 JURUPA AVE
 RIVERSIDE, CA 92504
 RIVERSIDE COUNTY
 SHEET TITLE

ENLARGED SITE PLAN AND ANTENNA PLAN

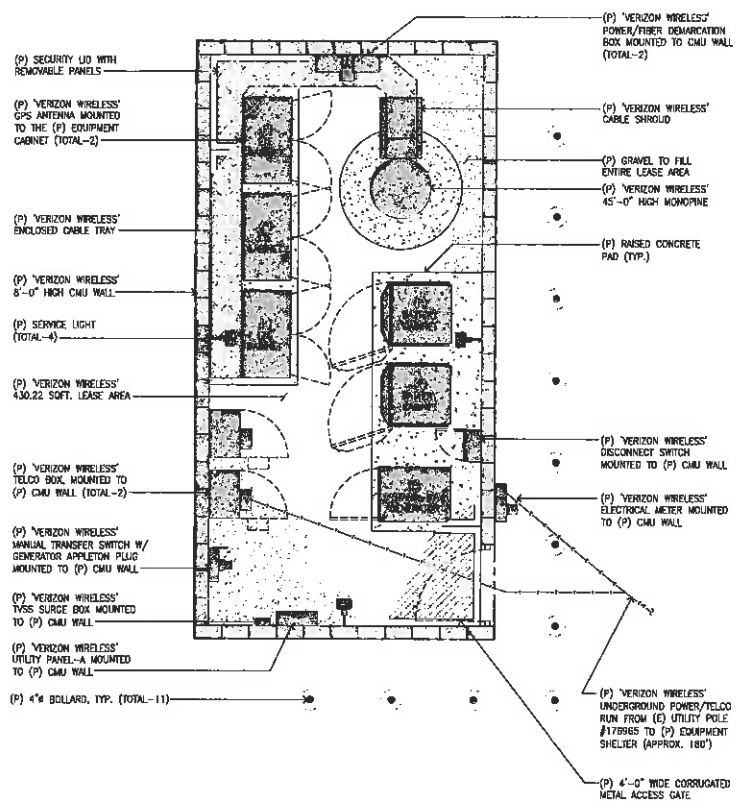
DRAWING INFORMATION
 DRAWN BY: AH
 CHECKED BY: APP
 ISSUE DATE: 07.09.2014
 SHEET NUMBER

A-2



DIMENSION PLAN

SCALE 3/8"=1'-0"
 0 1' 2' 4'



2 EQUIPMENT PLAN

SCALE 3/8"=1'-0"
 0 1' 2' 4'

REV	DATE/BY	DESCRIPTION
0	07/17/14	ISSUED FOR BIDDING
1	07/21/14	ISSUED FOR 100% BIDDING

ENGINEER / CONSULTANT

SITE BUILDER

15500 SAND CANYON AVENUE
 SUITE 200, FRESNO, CA 93720
 543.285.7000

ARC DEVELOPMENT

core

ARC SERVICES
 8749 Balboa Street
 Fresno, California 93821
 (714)728-8404 (714)323-4441 fax
 www.core.us.com

SITE DEVELOPMENT

core

DEVELOPMENT SERVICES
 8749 Balboa Street
 Fresno, California 93821
 (714)728-8404 (714)323-4441 fax
 www.core.us.com

SITE INFORMATION

REPUBLIC

SITE ADDRESS
 6600 JURUPA AVE.
 RIVERSIDE, CA 92504

RIVERSIDE COUNTY

SHEET TITLE

EQUIPMENT PLAN AND DIMENSION PLAN

DRAWING INFORMATION

DRAWN BY	CHECKED BY	ISSUE DATE
AH	APP	07.08.2014

SHEET NUMBER

A-3

REV	DATE / BY	DESCRIPTION
0	07/17/14 RD	ISSUED FOR SITE ZONING
1	07/21/14 RD	ISSUED FOR LOCAL ZONING

ENGINEER / CONSULTANT

SITE BUILDER



ARE DEVELOPMENT

core

ARE SERVICES

2745 Delano Street
Irvine, California 92617
(714)725-8444
www.core.us.com

SITE DEVELOPMENT

core

DEVELOPMENT SERVICES

3749 Station Road
Irvine, California 92617
(714)725-8444
www.core.us.com

SITE INFORMATION

SITE NAME
REPUBLIC

SITE ADDRESS
8800 JURUPA AVE.
RIVERSIDE, CA 92504
RIVERSIDE COUNTY

SHEET TITLE

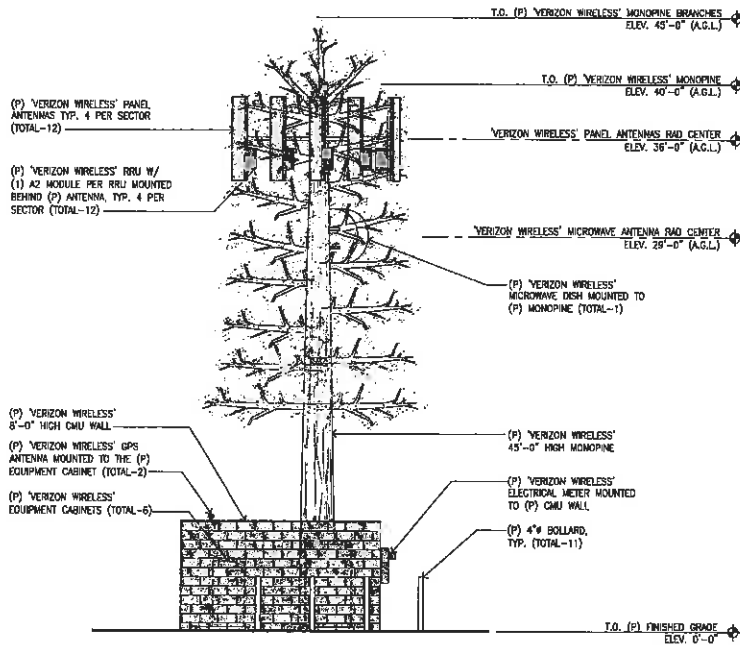
ELEVATIONS

DRAWING INFORMATION

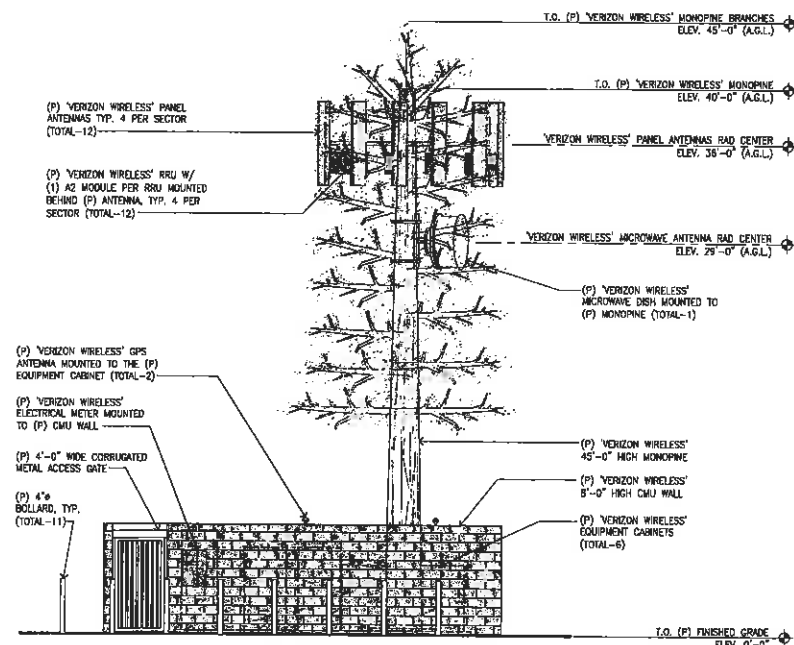
DRAWN BY	CHECKED BY	ISSUE DATE
AH	APP	07.09.2014

SHEET NUMBER

A-4



EAST ELEVATION



NORTH ELEVATION



PAGE BREAK





AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

August 13, 2015

CHAIR
Simon Housman
Rancho Mirage

VICE CHAIRMAN
Rod Ballance
Riverside

COMMISSIONERS

Arthur Butler
Riverside

Glen Holmes
Hemet

John Lyon
Riverside

Greg Pettis
Cathedral City

Steve Manos
Lake Elsinore

STAFF

Director
Ed Cooper

John Guerin
Russell Brady
Barbara Santos

County Administrative Center
4080 Lemon St., 14th Floor
Riverside, CA 92501
(951) 955-5132

www.rcaluc.org

Mr. Dave Alvarez, Project Planner
City of Murrieta Planning Department
One Town Square
25601 Jefferson Avenue
Murrieta, CA 92562

RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW

File No.: ZAP1065FV15
Related File No.: MCUP 2015-591
APN: 900-020-005

Dear Mr. Alvarez:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC), staff reviewed the above-referenced proposal for the establishment of an unmanned telecommunications facility consisting of antennas on a 55-foot high mono-eucalyptus, with associated equipment enclosure and related facilities, on a 546 square foot lease area within a 4.58-acre property located on the northerly side of Crawford Canyon Road, westerly of its intersection with Jessie Ceas Lane, in the City of Murrieta.

The site is located within Airport Compatibility Zone E of the French Valley Airport Influence Area (AIA). Given the site's proximity to the runway (approximately 7,879 feet) and relative elevation, the applicant filed Form 7460-1 with the Federal Aviation Administration Obstruction Evaluation Service and obtained a "Determination of No Hazard to Air Navigation."

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2007 French Valley Airport Land Use Compatibility Plan (as amended in 2011), subject to the following conditions:

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky.
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved

navigational signal light or visual approach slope indicator.

- (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area, including but not limited to, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, artificial marshes, incinerators, and fly ash disposal.
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached notice shall be provided to all potential purchasers and lessees of the property and tenants of the residence and any habitable buildings thereon.
 4. The Federal Aviation Administration has conducted an aeronautical study of the proposed structure (Aeronautical Study No. 2015-AWP-191-OE) and has determined that neither marking nor lighting of the structure is necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 K Change 2 and shall be maintained in accordance therewith for the life of the project.
 5. The maximum height of the proposed structure shall not exceed 55 feet above ground level, and the maximum elevation at the top of the proposed structure shall not exceed 1,484 feet above mean sea level.
 6. The specific coordinates, height, top point elevation of the proposed structure, frequencies, and power shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in building height or elevation shall not require further review by the Airport Land Use Commission.
 7. Temporary construction equipment used during actual construction of the structure shall not exceed the height of the structure, unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.
 8. Within five (5) days after construction of the structure reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned.

9. The telecommunications facility shall be designed in such a manner as to ensure that spurious emissions signal levels from the proposed transmitter(s) will be less than -104 dBm in the 108-137 and 225-400 MHz frequency bands at a distance of 7,800 feet from the transmitter site, in accordance with the requirements of the Federal Aviation Administration Obstruction Evaluation Service letter dated January 29, 2015, a copy of which is attached hereto and incorporated herein by reference.

10. The following condition must be included *verbatim* on the building permit, as a condition to the Determination of No Hazard (see Page 2 of the FAA letter issued on January 29, 2015):

“Upon receipt of notification from the Federal Communications Commission that harmful interference is being caused by the permittee’s transmitter, the permittee shall either immediately reduce the power to the point of no interference, cease operation, or take such immediate corrective action as is necessary to eliminate the harmful interference. This condition expires after one year of interference-free operation.”

If you have any questions, please contact Russell Brady, Contract Planner, at (951) 955-0549, or John Guerin, Principal Planner, at (951) 955-0982.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Edward C. Cooper, Director

RB/JJGJG

Attachments: Notice of Airport in Vicinity
FAA Aeronautical Study No. 2015-AWP-191-OE

cc: Core Development Services – Attn.: Henry Castro (applicant/representative)
Kenneth and Beth Whitlock (property owners)
Daryl Shippy, Airports Manager, Riverside County EDA – Aviation Division
ALUC Case File

Y:\AIRPORT CASE FILES\French Valley\ZAP1065FV15\ZAP1065FV15LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 2601 Meacham Boulevard
 Fort Worth, TX 76193

Aeronautical Study No.
 2015-AWP-191-OE

Issued Date: 01/29/2015

Candice Koenig
 Los Angeles SMSA Limited Partnership
 1120 Sanctuary Pkwy, #150 GASA5REG
 Alpharetta, GA 30009

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Monopole Manana
 Location: Murrieta, CA
 Latitude: 33-35-47.44N NAD 83
 Longitude: 117-08-52.41W
 Heights: 1429 feet site elevation (SE)
 55 feet above ground level (AGL)
 1484 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed and maintained in accordance with FAA Advisory circular 70/7460-1 K Change 2.

This determination expires on 07/29/2016 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination of No Hazard is granted provided the following conditional statement is included in the proponent's construction permit or license to radiate:

Upon receipt of notification from the Federal Communications Commission that harmful interference is being caused by the licensee's (permittee's) transmitter, the licensee (permittee) shall either immediately reduce the power to the point of no interference, cease operation, or take such immediate corrective action as is necessary to eliminate the harmful interference. This condition expires after 1 year of interference-free operation.

This determination is based, in part, on the foregoing description which includes specific coordinates , heights, frequency(ies) and power . Any changes in coordinates , heights, and frequencies or use of greater power will void this determination. Any future construction or alteration , including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

A copy of this determination will be forwarded to the Federal Communications Commission (FCC) because the structure is subject to their licensing authority.

If we can be of further assistance, please contact our office at (425) 227-2625. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2015-AWP-191-OE.

Signature Control No: 239362301-241772387
Paul Holmquist
Technician

(DNE)

Attachment(s)
Additional Information
Frequency Data
Map(s)

cc: FCC

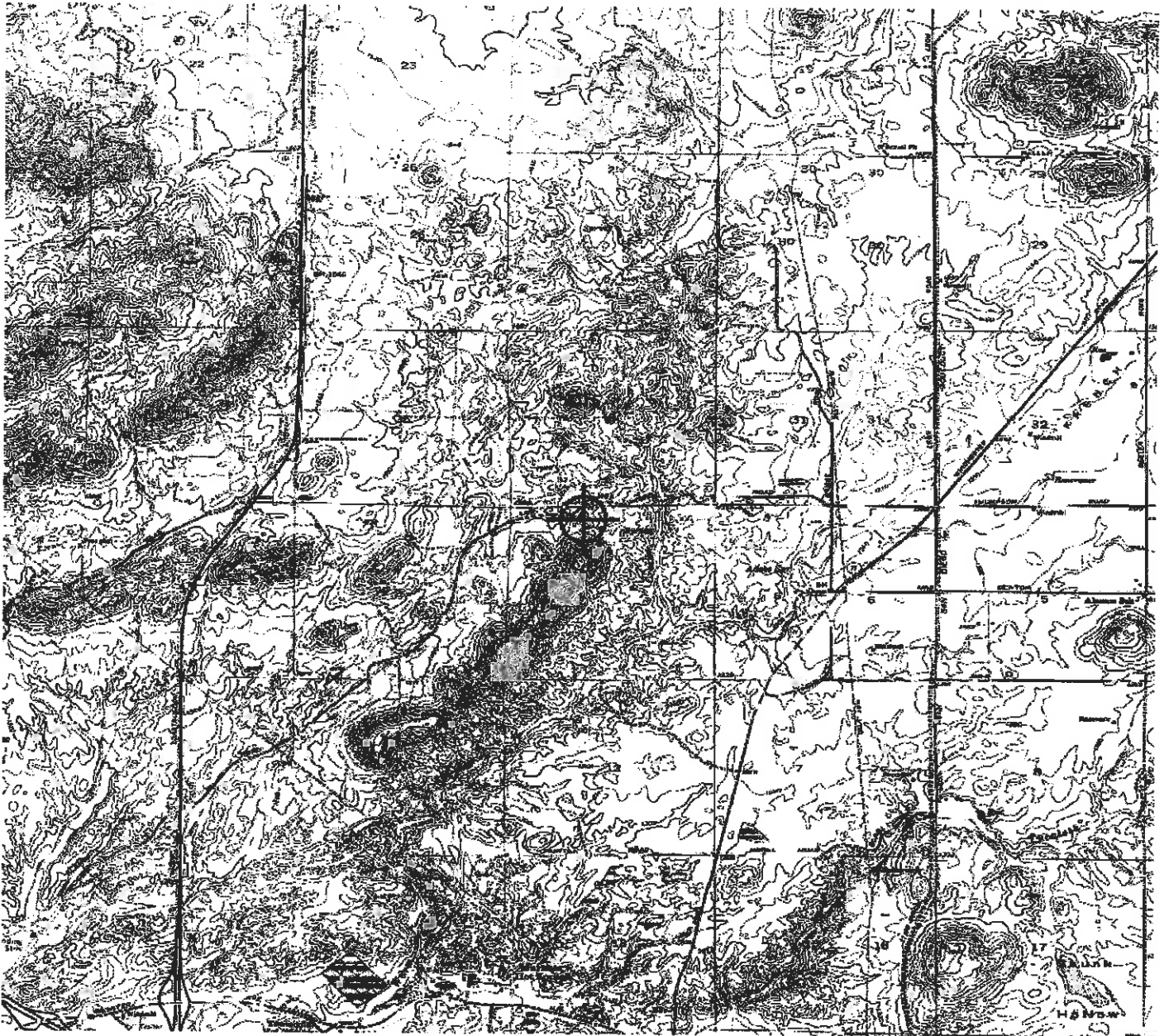
Additional information for ASN 2015-AWP-191-OE

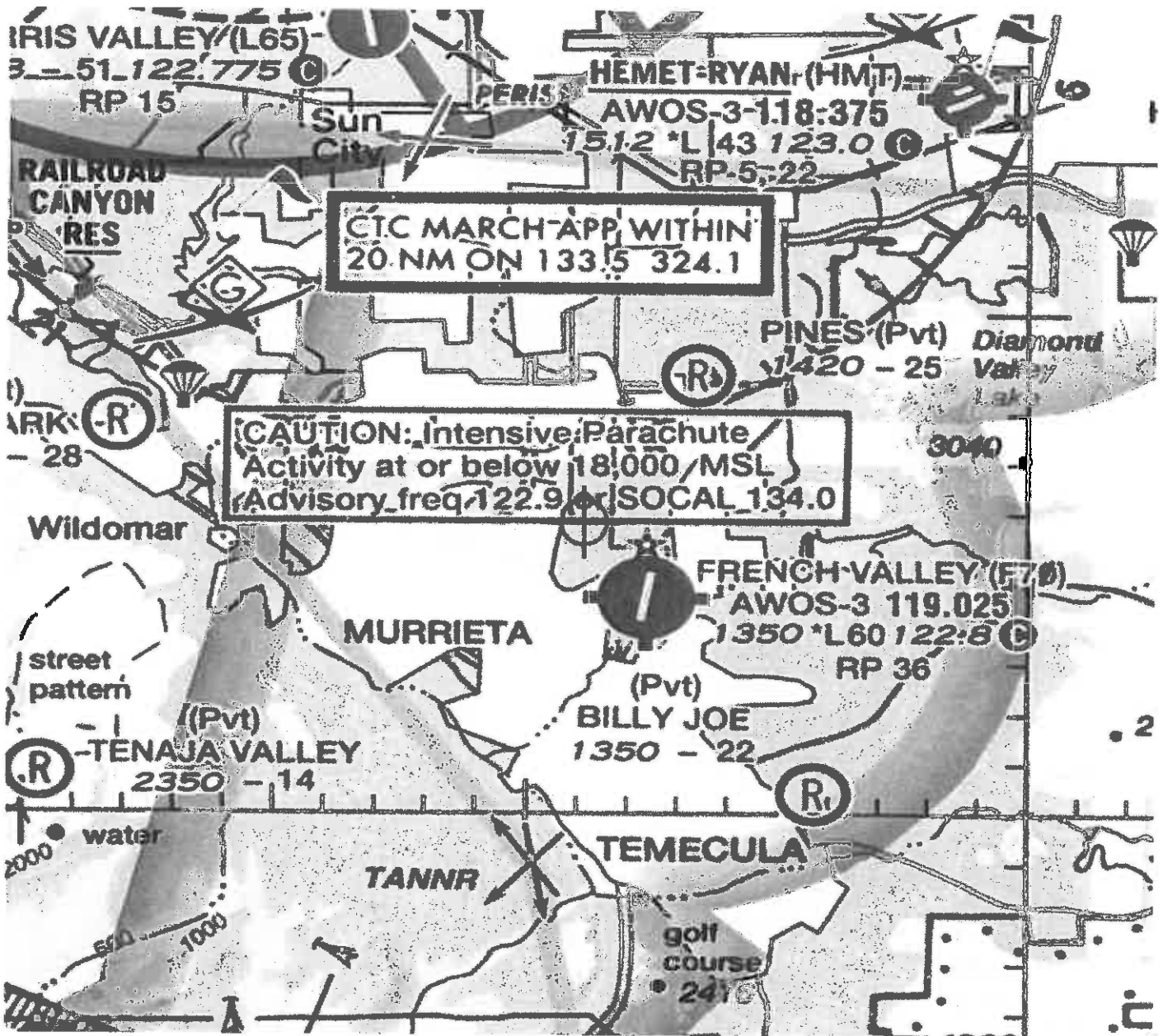
At a distance of 7800 feet from transmitter site spurious emissions signal levels from proposed transmitters must be less than -104 dBm in the 108-137, 225-400 MHz frequency bands.

Frequency Data for ASN 2015-AWP-191-OE

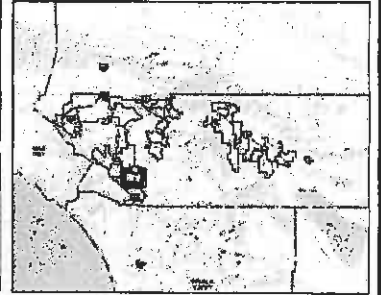
LOW FREQUENCY	HIGH FREQUENCY	FREQUENCY UNIT	ERP	ERP UNIT
698	806	MHz	1000	W
806	824	MHz	500	W
824	849	MHz	500	W
851	866	MHz	500	W
869	894	MHz	500	W
896	901	MHz	500	W
901	902	MHz	7	W
930	931	MHz	3500	W
931	932	MHz	3500	W
932	932.5	MHz	17	dBW
935	940	MHz	1000	W
940	941	MHz	3500	W
1850	1910	MHz	1640	W
1930	1990	MHz	1640	W
2305	2310	MHz	2000	W
2345	2360	MHz	2000	W

TOPO Map for ASN 2015-AWP-191-OE





My Map



Legend

-  AIA
-  Runways
-  City Boundaries



0 5,794 11,589 Feet



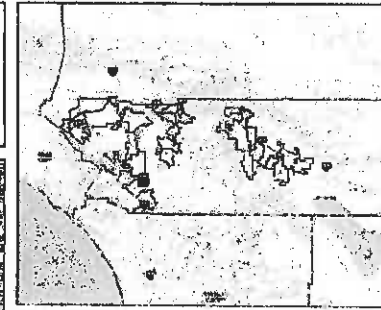
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 8/13/2015 5:25:37 PM

© Riverside County TLMA GIS

Notes

My Map



Legend

- RCLIS Parcels
- AIA
- Runways
- City Boundaries
- roadsanno
- highways
 - HWY
 - INTERCHANGE
 - INTERSTATE
 - OFFRAMP
 - ONRAMP
 - USHWY
- counties
- cities
- hydrographylines
- waterbodies
 - Lakes
 - Rivers

City of Murrieta

City of Murrieta



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

0 1,449 2,897 Feet



REPORT PRINTED ON... 8/7/2015 5:22:56 PM

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Notes

CODE COMPLIANCE

2013 CALIFORNIA BUILDING CODE
2013 CALIFORNIA MECHANICAL CODE
2013 CALIFORNIA TITLE 24
2013 CALIFORNIA FIRE CODE
2013 CALIFORNIA RESIDENTIAL CODE
2013 CALIFORNIA ENERGY CODE
2013 CALIFORNIA PLUMBING CODE

2013 CALIFORNIA MECHANICAL CODE
2013 CALIFORNIA ELECTRIC CODE
2014 IBC WITH AMENDMENTS
ANSI/ISA-226-G OR LATEST EDITION
LOCAL CODES AND AMENDMENTS

GENERAL NOTES

THE FACILITY IS UNMANNED AND NOT FOR HUMAN HABITATION. A TECHNICIAN WILL VISIT THE SITE AS REQUIRED FOR ROUTINE MAINTENANCE. THE PROJECT WILL NOT RESULT IN ANY SIGNIFICANT DISTURBANCE OR EFFECT ON DRAINAGE, NO SANITARY SEWER SERVICE, POTABLE WATER, OR TRASH DISPOSAL IS REQUIRED AND NO COMMERCIAL SIGNAGE IS PROPOSED.



DEAL TOLL FREE
877 1

150 SOUTH UNDERWOOD DRIVE SUITE 2000 BREA, CA 92615

SITE INFORMATION

PROPERTY OWNER: KENNETH AND BETH WENTLOCK
29400 CRAWFORD CANYON ROAD
MURRIETA, CA 92563
CONTACT: KENNETH WENTLOCK
PHONE: (951) 551-5087

APPLICANT: VERIZON WIRELESS
15505 SAND CANYON AVE.
IRVINE, CA 92618

APPLICANT REPRESENTATIVE: CORE DEVELOPMENT SERVICES
2749 SATURN STREET
BREA, CA 92821
CONTACT: ROSANNA ASHBY
PHONE: (714) 896-6372
EMAIL: msap@cs@coresd.com

GROUND ELEVATION: 4143.25' A.S.M.L.
APN #: 905-005-005-6
ZONING JURISDICTION: CITY OF MURRIETA
CURRENT ZONING: R-R (RURAL RESIDENTIAL)
CONSTRUCTION TYPE: V-B
OCCUPANCY: U-UNMANNED TELECOMMUNICATIONS FACILITY
POWER COMPANY: SCE
(800) 685-4555
TELCO COMPANY: CDX COMMUNICATION
(865) 570-5499
OCCUPANCY: U-UNMANNED TELECOMMUNICATIONS FACILITY

EQUIPMENT LEASE AREA: 220 SQ. FT.
TOTAL EQUIPMENT & ANTENNA LEASE AREA: 548 SQ. FT.



MANANA

**PROJECT TYPE:
NEW SITE BUILD
(MACRO CELL EVOLUTION DESIGN)**

**PROJECT ADDRESS:
29400 CRAWFORD CANYON ROAD
MURRIETA, CA 92563**

ZONING DRAWING

IF USING 11'X17" PLOT, DRAWINGS WILL BE HALF SCALE

APPROVALS

THE FOLLOWING PARTIES HEREBY APPROVE AND ACCEPT THESE DOCUMENTS & AUTHORIZE THE SUBCONTRACTOR TO PROCEED WITH THE CONSTRUCTION DESCRIBED HEREIN. ALL DOCUMENTS ARE SUBJECT TO REVIEW BY THE LOCAL BUILDING DEPARTMENT & MAY IMPOSE CHANGES OR MODIFICATIONS.

VERIZON WIRELESS RF ENGINEER: _____
VERIZON WIRELESS OPERATIONS: _____
SITE ACQUISITION MANAGER: _____
PROJECT MANAGER: _____
ZONING VENDOR: _____
LEASING VENDOR: _____
CONSTRUCTION MANAGER: _____
A/E MANAGER: _____
PROPERTY OWNER: _____

PROJECT DESCRIPTION

VERIZON WIRELESS PROPOSES THE CONSTRUCTION OF A NEW TELECOMMUNICATIONS FACILITY, THE SCOPE WILL CONSIST OF THE FOLLOWING:

- INSTALL PROPOSED VERIZON WIRELESS HIGH C.M.U. EQUIPMENT ENCLOSURE WITH CONCRETE SLAB AND BASES/ EQUIPMENT CURBS.
- INSTALL (12) PROPOSED VERIZON WIRELESS PANEL ANTENNAS.
- INSTALL (12) PROPOSED VERIZON WIRELESS IPDS RELOCATE RAINNO LIMITS.
- INSTALL (2) PROPOSED VERIZON WIRELESS BANDS ON POLE.
- INSTALL (1) PROPOSED VERIZON WIRELESS MICROWAVE DISH.
- INSTALL (1) PROPOSED VERIZON WIRELESS DC OPERATOR CABINET.
- INSTALL (2) PROPOSED VERIZON WIRELESS MICE EQUIPMENT CABINETS.
- INSTALL (2) PROPOSED VERIZON WIRELESS GPS ANTENNAS.
- INSTALL (1) PROPOSED VERIZON WIRELESS METER.
- INSTALL (24) PROPOSED VERIZON WIRELESS 7/8" COAX CABLES, (2) PIER ANTENNA.
- INSTALL (1) PROPOSED VERIZON WIRELESS 85'-0" HIGH MONO-EUCALYPTUS.
- INSTALL (1) PROPOSED VERIZON WIRELESS TELCO P.O.C.
- INSTALL (1) PROPOSED VERIZON WIRELESS 24'-0" WIDE DOUBLE SWING ACCESS GATE.
- REMOVE PORTION OF EXISTING CHAIN LINK FENCE WITH BARR WIVES.

DRAWING INDEX

SHEET NO:	TITLE SHEET	SHEET TITLE
T-1	TITLE SHEET	
LS-1	TOPOGRAPHIC SURVEY	
LS-2	TOPOGRAPHIC SURVEY	
A-1	SITE PLAN	
A-2	ENLARGED SITE PLAN	
A-3	PROPOSED EQUIPMENT LAYOUT PLAN	
A-4	PROPOSED ANTENNA LAYOUT PLANS	
A-5	ARCHITECTURAL ELEVATIONS	
A-6	ARCHITECTURAL ELEVATIONS	

DO NOT SCALE DRAWINGS

SUBCONTRACTOR SHALL VERIFY ALL PLANS & EXISTING DIMENSIONS & CONDITIONS ON THE JOB SITE & SHALL IMMEDIATELY NOTIFY THE ENGINEER IN WRITING OF ANY DISCREPANCIES BEFORE PROCEEDING WITH THE WORK OR BE RESPONSIBLE FOR SAME.

PROJECT TEAM

PROJECT MANAGER: VERIZON WIRELESS
15505 SAND CANYON AVE.
IRVINE, CA 92618
CONTACT: GUY SPANGLER
PHONE: (949) 236-8448
guy.spangler@verizonwireless.com

ENGINEERING/SURVEY: NATIONAL ENGINEERING & CONSULTING, INC.
27 BUCHFORD
LAKE FOREST, CA 92653
PHONE: (949) 716-9999
FAX: (949) 716-9997

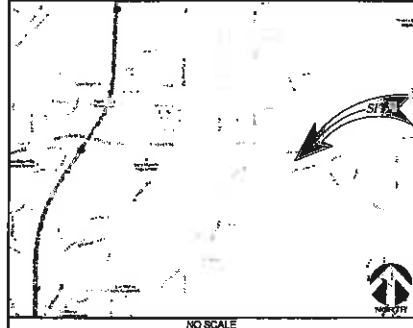
ZONING: CORE DEVELOPMENT SERVICES
2749 SATURN STREET
BREA, CA 92821
CONTACT: ROSANNA ASHBY
PHONE: (714) 308-0054
EMAIL:

RF ENGINEER: VERIZON WIRELESS
15505 SAND CANYON AVE.
IRVINE, CA 92618
CONTACT: FERNANDO CARRANZA
PHONE: (919) 906-2706

LEASING MANAGER: CORE DEVELOPMENT SERVICES
2749 SATURN STREET
BREA, CA 92821
CONTACT: ROSANNA ASHBY
PHONE: (714) 392-8032
EMAIL:

CONSTRUCTION: VERIZON WIRELESS
15505 SAND CANYON AVE.
IRVINE, CA 92618
CONTACT: JOHN DANG
PHONE: (949) 422-4471
EMAIL: john.dang@verizonwireless.com

VICINITY MAP



DRIVING DIRECTIONS

DIRECTIONS FROM VERIZON WIRELESS OFFICE:

- DEPART SAND CANYON AVE TOWARD BARRANCA PARKWAY.
- TURN LEFT ONTO BARRANCA PARKWAY.
- TURN RIGHT ONTO PACIFIC.
- TAKE RAMP RIGHT AND FOLLOW SIGNS FOR CA-133 NORTH.
- KEEP STRAIGHT ONTO I-48.
- TAKE RAMP LEFT FOR I-48-241 N.
- AT EXIT 568, TAKE RAMP RIGHT FOR CA-91 EAST TOWARD RIVERSIDE.
- TAKE RAMP RIGHT FOR I-15 SOUTH TOWARD SAN DIEGO.
- AT EXIT 64, TAKE RAMP RIGHT AND FOLLOW SIGNS FOR MURRIETA HOT SPRING RD.
- TAKE RAMP RIGHT FOR I-215 TOWARD RIVERSIDE.
- AT EXIT 2, TAKE RAMP RIGHT AND FOLLOW SIGNS FOR LOS ALAMOS ROAD.
- TURN RIGHT ONTO LOS ALAMOS RD.
- TURN RIGHT ONTO CRAWFORD CANYON ROAD.

DESTINATION WILL BE ON THE LEFT.
29400 CRAWFORD CANYON ROAD,
MURRIETA, CA 92563



PLANS PREPARED BY:



CLIENTS

NO.	DATE	DESCRIPTION	BY
1	09/25/14	90% ZD	SP
2	10/07/14	100% ZD	SP
3	10/23/14	REVISED 100% ZD	RAG
4	04/07/15	RELOCATE SITE BOX ZD	MM
5	04/15/15	100% ZD	JY
6	04/28/15	SAG COMMENT	RAG

SITE INFORMATION:

MANANA

29400 CRAWFORD CANYON ROAD,
MURRIETA, CA 92563

SCALE:

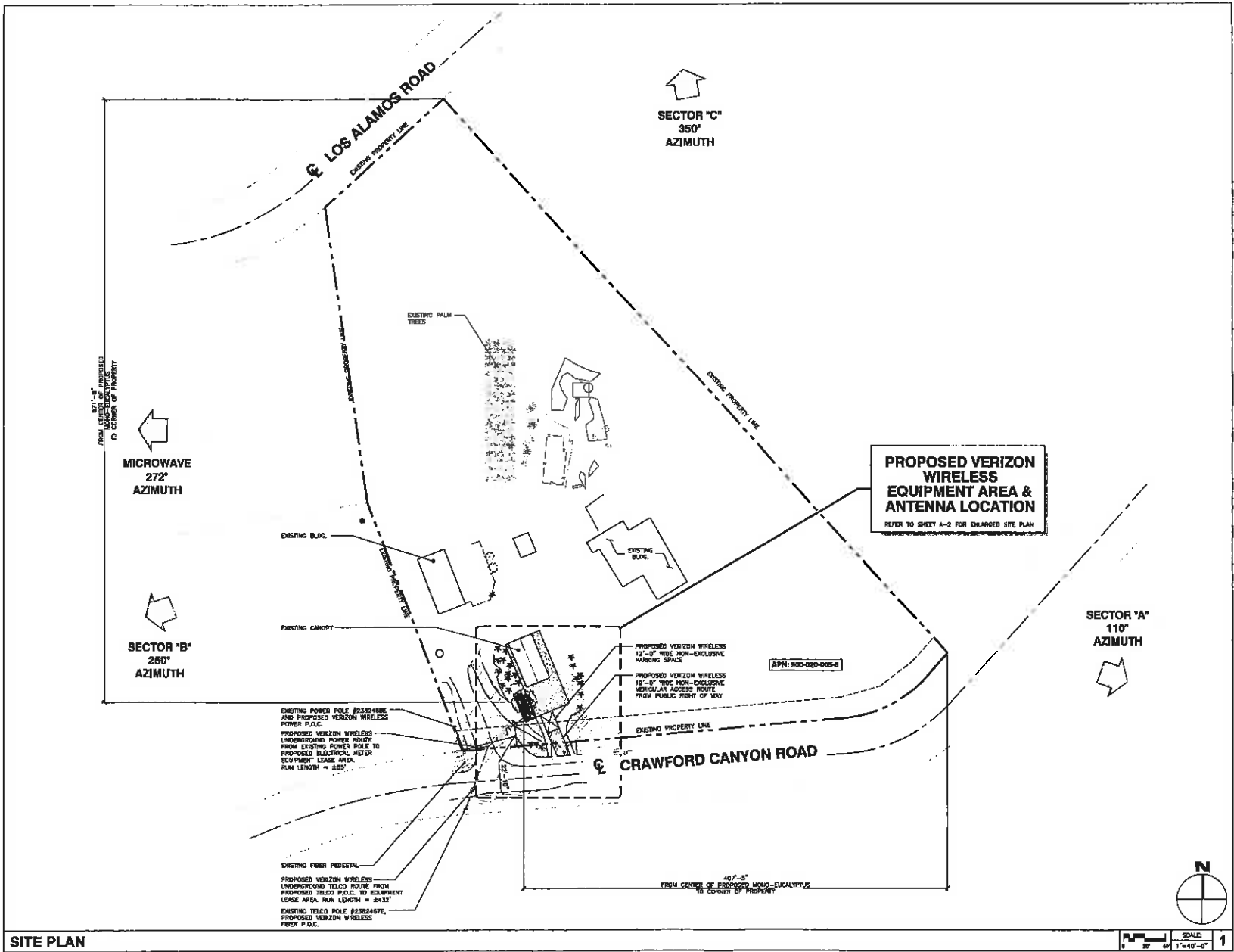
SHEET TITLE:

TITLE SHEET

SHEET NUMBER:

T-1

FU
ZAPLO6



SITE PLAN



PLANS PREPARED BY:

NATIONAL
ENGINEERING & CONSULTING, INC.
2717 RICHMOND LANE, FORTY-FIVE, CA 94503
PHONE: (925) 715-9999 | FAX: (925) 715-9997

CLIENTS

NO.	DATE	DESCRIPTION	BY
1	05/25/14	90% ZD	SP
2	10/07/14	100% ZD	SP
3	10/23/14	REVISED 100% ZD	RAG
4	04/07/15	RELOCATE SITE 90% ZD	NM
5	04/15/15	100% ZD	JY
6	04/28/15	SAQ COMMENT	RAG

SITE INFORMATION:

MANANA
2940 CRAWFORD CANYON ROAD,
MURRIETA, CA 92583

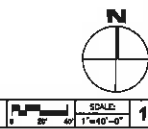
SEAL:

SHEET TITLE:

SITE PLAN

SHEET NUMBER:

A-1





1936 SAND CANYON AVE.
SUNNYVALE, CA 95088

PLANS PREPARED BY:
NATIONAL
ENGINEERING & CONSULTING, INC.
BY: DAN CHARNICK, LAND SURVEYOR, CIVIL ENGINEER
PHOTOGRAPHIC SURVEY, PHOTOGRAMMETRY, REMOTE SENSING
PHONETIC (949) 271-8200 | FAX: (949) 271-0207

CLIENTS:

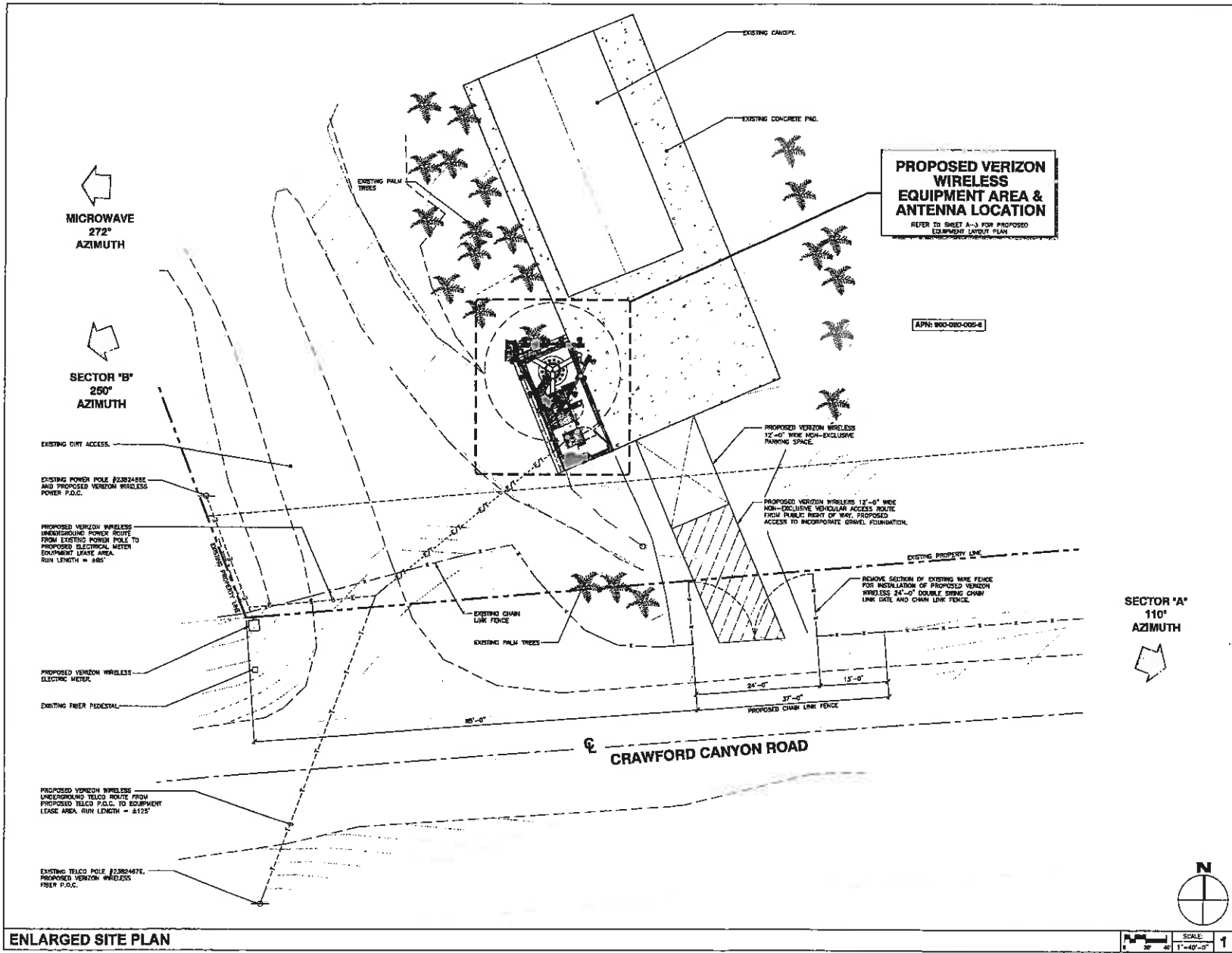
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2	10/07/14	100% ZD	SP
3	10/23/14	REVISED 100% ZD	RAG
4	04/07/15	RELOCATE SITE	MM
5	04/15/15	100% ZD	JY
6	04/28/15	SAQ COMMENT	RAG

SITE INFORMATION:
MANANA
29400 CRAWFORD CANYON ROAD,
MURRIETA, CA 92563

SCALE:

SHEET TITLE:
ENLARGED SITE PLAN

SHEET NUMBER:
A-2



PROPOSED VERIZON WIRELESS EQUIPMENT AREA & ANTENNA LOCATION
REFER TO SHEET A-3 FOR PROPOSED EQUIPMENT LAYOUT PLAN

APN: 900-080-005-8

SECTOR 'A'
110°
AZIMUTH



SCALE: 1"=40'-0"

ENLARGED SITE PLAN



PLANS PREPARED BY:



CLIENTS:

NO.	DATE	DESCRIPTION	BY
1	09/25/14	90% ZD	SP
2	10/07/14	100% ZD	SP
3	10/23/14	REVISED 100% ZD	RAG
4	04/07/15	RELOCATE SITE 90% ZD	MM
5	04/15/15	100% ZD	JY
6	04/26/15	SAD COMMENT	RAG

SITE INFORMATION:

MANANA

29400 CRAWFORD CANYON ROAD,
MURRIETA, CA 92583

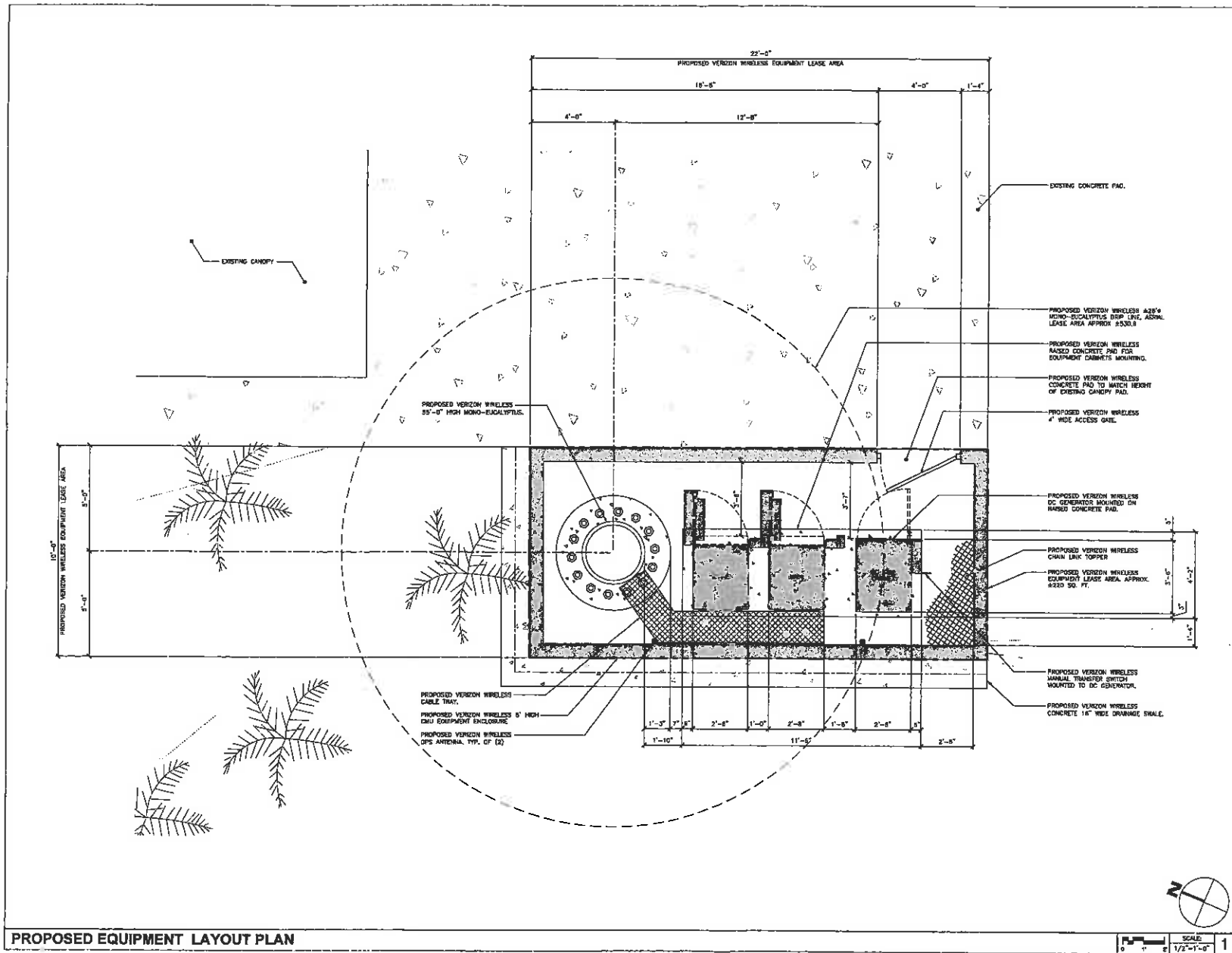
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SHEET TITLE:

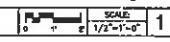
PROPOSED EQUIPMENT LAYOUT PLAN

SHEET NUMBER:

A-3



PROPOSED EQUIPMENT LAYOUT PLAN



CLIENTS

NO.	DATE	DESCRIPTION	BY
1	09/25/14	90% ZD	SP
2	10/07/14	100% ZD	SP
3	10/23/14	REVISED 100% ZD	RAG
4	04/07/15	RELOCATE SITE 90% ZD	MM
5	04/15/15	100% ZD	JY
6	04/28/15	SAO COMMENT	RAG

SITE INFORMATION:

MANANA

2940 CRAWFORD CANYON ROAD,
 MURRIETA, CA 92583

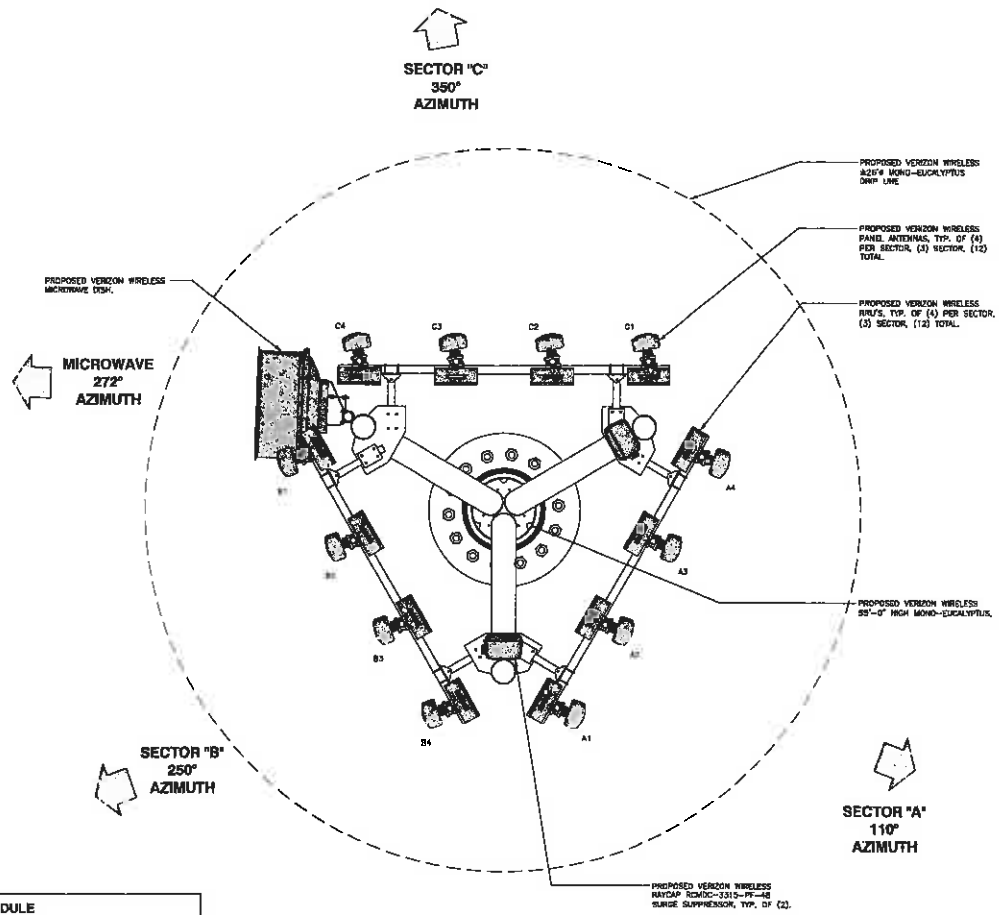
SEAL:

SHEET TITLE:

PROPOSED ANTENNA LAYOUT PLANS

SHEET NUMBER:

A-4



COAX SCHEDULE				
ANTENNA SECTOR	ANTENNA AZIMUTH	ANTENNA MODEL	COAX LENGTH	COAX CABLE SIZE
A	110°	T80	±185'	7/8"
B	250°	T80	±85'	7/8"
C	350°	T80	±185'	7/8"
GPS	N/A	T80	-	1/2"

NOTE: CONSTRUCTION MANAGER TO FIELD VERIFY CABLE LENGTHS PRIOR TO ORDERING, FABRICATION OR INSTALLATION OF CABLES. CHECK W/ DATA SHEET.

PROPOSED ANTENNA LAYOUT PLAN

Scale: 1/4" = 1'-0"

North Arrow

PLANS PREPARED BY:

NATIONAL
ENGINEERING & CONSULTING, INC
87 COUNCHMAN LAKE PARKWAY, CA 95008
PHONE: (925) 71-8888 | FAX: (925) 71-8889

CLIENTS

NO.	DATE	DESCRIPTION	BY:
1	09/25/14	90% ZD	SP
2	10/07/14	100% ZD	SP
3	10/23/14	REVISED 100% ZD	RAG
4	04/07/15	RELOCATE SITE BOX ZD	MM
5	04/15/15	100% ZD	JY
6	04/26/15	SAO COMMENT	RAG

SITE INFORMATION:

MANANA

25400 CRAWFORD CANYON ROAD,
MURFREESBORO, TN 37130

SCALE:

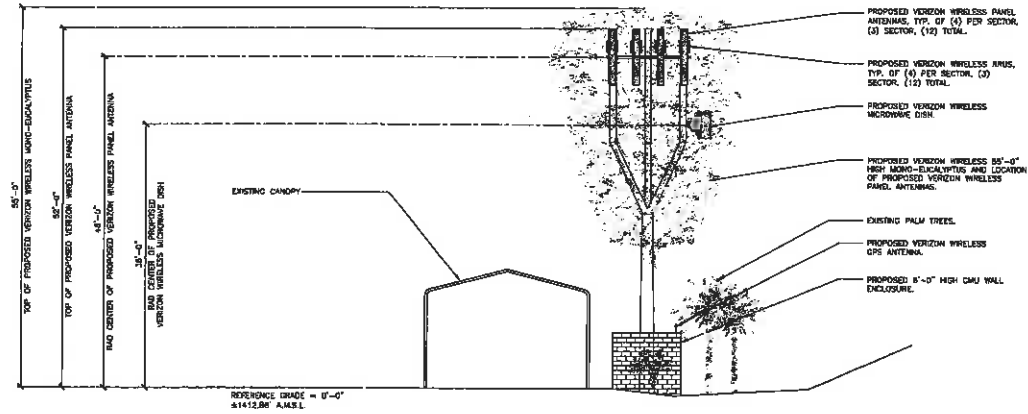
SHEET TITLE:

**ARCHITECTURAL
ELEVATIONS**

SHEET NUMBER:

A-5

SECTOR 'C'
350°
AZIMUTH

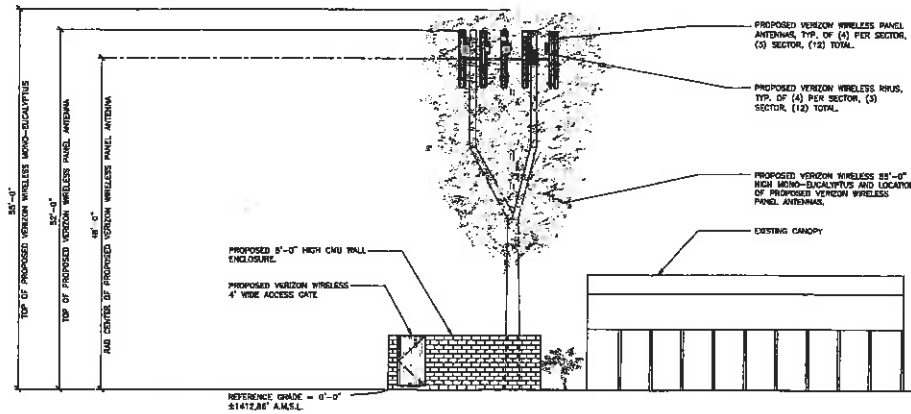


PROPOSED NORTHWEST ELEVATION



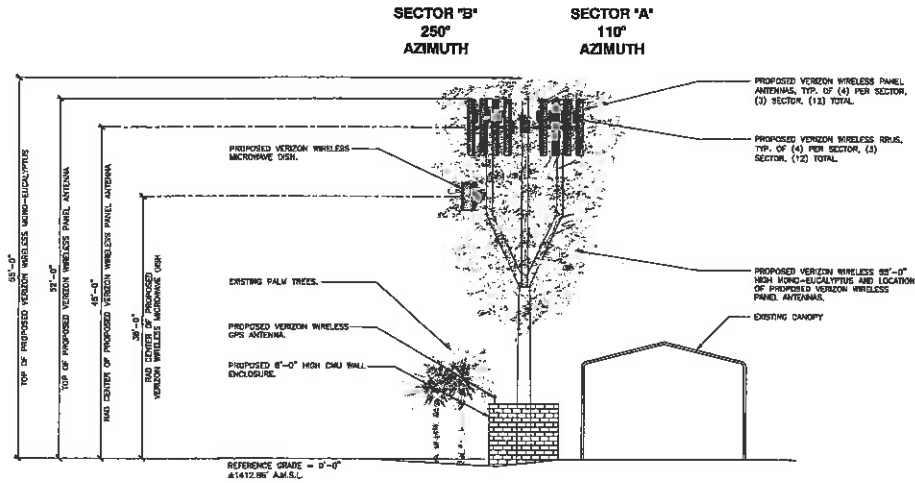
SECTOR 'A'
110°
AZIMUTH

SECTOR 'C'
350°
AZIMUTH



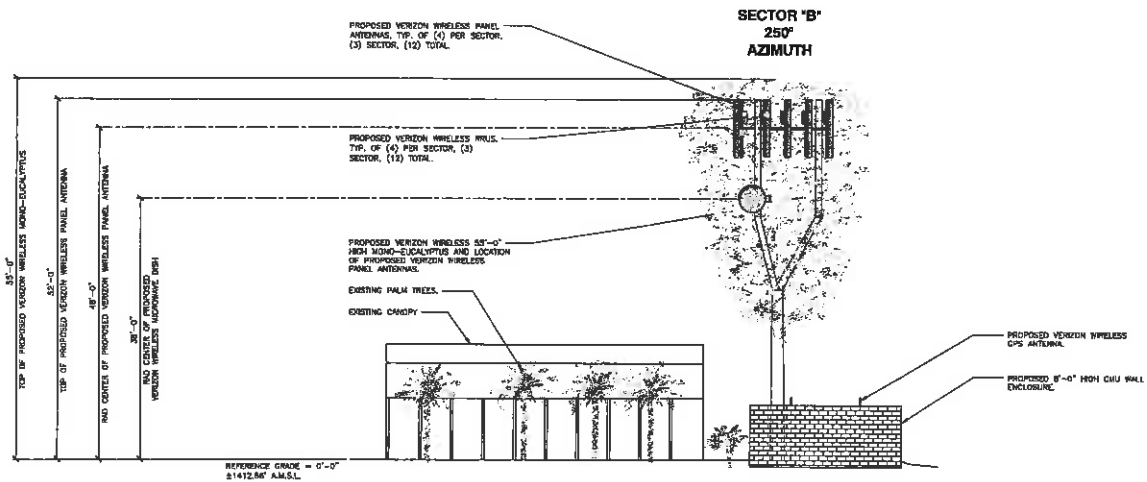
PROPOSED NORTHEAST ELEVATION





PROPOSED SOUTHEAST ELEVATION

SCALE: 1/8" = 1'-0" 1



PROPOSED SOUTHWEST ELEVATION

SCALE: 1/8" = 1'-0" 2



1800 SAND CANYON AVE
POYNE, CA 92911

PLANS PREPARED BY:



ENGINEERING & CONSULTING, INC.
27 ORCHARD LAKE POYNEY, CA 92920
PHONE: (949) 718-0881 FAX: (949) 718-0977

CLIENTS

NO.	DATE	DESCRIPTION	BY
1	09/25/14	90% ZD	SP
2	10/07/14	100% ZD	SP
3	10/23/14	REVISED 100% ZD	RAG
4	04/07/15	RELOCATE SITE BOX ZD	MM
5	04/15/15	100% ZD	JY
6	04/28/15	SAO COMMENT	RAG

SITE INFORMATION:

MANANA

20400 CRAWFORD CANYON ROAD,
MURBETA, CA 92953

SEAL:

SHEET TITLE:
ARCHITECTURAL ELEVATIONS

SHEET NUMBER:
A-6

PAGE BREAK



**AIRPORT LAND USE COMMISSION
RIVERSIDE COUNTY**



CHAIR

Simon Housman
Rancho Mirage

August 31, 2015

VICE CHAIRMAN

Rod Ballance
Riverside

Ms. Candice Assadzadeh, Assistant Planner
City of Riverside Community Development Department - Planning Division
3900 Main Street, Third Floor
Riverside, CA 92522

COMMISSIONERS

Arthur Butler
Riverside

RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW

Glen Holmes
Hemet

File No.: ZAP1139MA15
Related File No.: P15-0483 (Conditional Use Permit)
APN: 276-110-025

John Lyon
Riverside

Dear Ms. Assadzadeh:

Greg Pettis
Cathedral City

Pursuant to the project-specific delegation of the Riverside County Airport Land Use Commission (ALUC) authorized at its August 13, 2015 meeting, staff reviewed the above-referenced proposal to re-establish a convenience store and service station and retain an existing drive-thru on a 0.72-acre property located on the westerly side of Mission Grove Parkway South, southerly of its intersection with Alessandro Boulevard in the City of Riverside.

Steve Manos
Lake Elsinore

STAFF

Director
Ed Cooper

The site is located within Airport Compatibility Zone C2 of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Airport Compatibility Zone C2 allows up to 200 people per acre. Thus, the site could potentially accommodate 144 persons. On the basis of one person per 115 square feet, the 3,400 square foot convenience store is projected to be occupied by 30 persons. The mezzanine area (993 square feet) is used only for storage. Even adding customers at the gas pumps, actual occupancy is anticipated to be less than 100 persons. The site provides 32 parking spaces, indicating a potential occupancy of 48 persons using the Parking Space Method.

John Guerin
Russell Brady
Barbara Santos

County Administrative Center
4080 Larran St., 14th Floor.
Riverside, CA 92501
(951) 955-5132

www.rcaluc.org

No new structures or increases in structure height are proposed through this application.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, subject to the following conditions:

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky.

2. The following uses are prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area, including but not limited to, landscaping utilizing water features, composting operations, trash transfer stations that are open on one or more sides, and recycling centers containing putrescible wastes.
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached notice shall be provided to all prospective purchasers of the property and tenants of the building, and shall be recorded as a deed notice.
4. Future changes in tenancy of this building to establish a different type of retail facility or office shall not require ALUC review. However, the City of Riverside shall submit any proposal to establish a more intensive use to ALUC for review. These more intensive uses would be uses that permit more than one occupant per 30 square feet pursuant to the Uniform Building Code (minimum square feet per occupant less than 30), and include, but are not necessarily limited to, the following:

Assembly areas, churches and places of worship, dance floors, fraternal lodges, conference facilities, restaurants, bars and cocktail lounges, gymnasiums, stages, gaming, auction rooms, classrooms.
5. No new detention basins are proposed through this application, nor are any such basins included in this consistency determination.
6. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.

If you have any questions, please contact John Guerin, Principal Planner, at (951) 955-0982.

AIRPORT LAND USE COMMISSION

August 31, 2015

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Edward C. Cooper, Director

JJGJG

Attachments: Notice of Airport in Vicinity

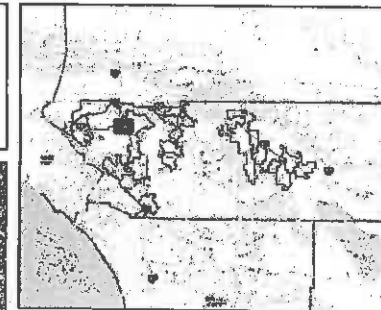
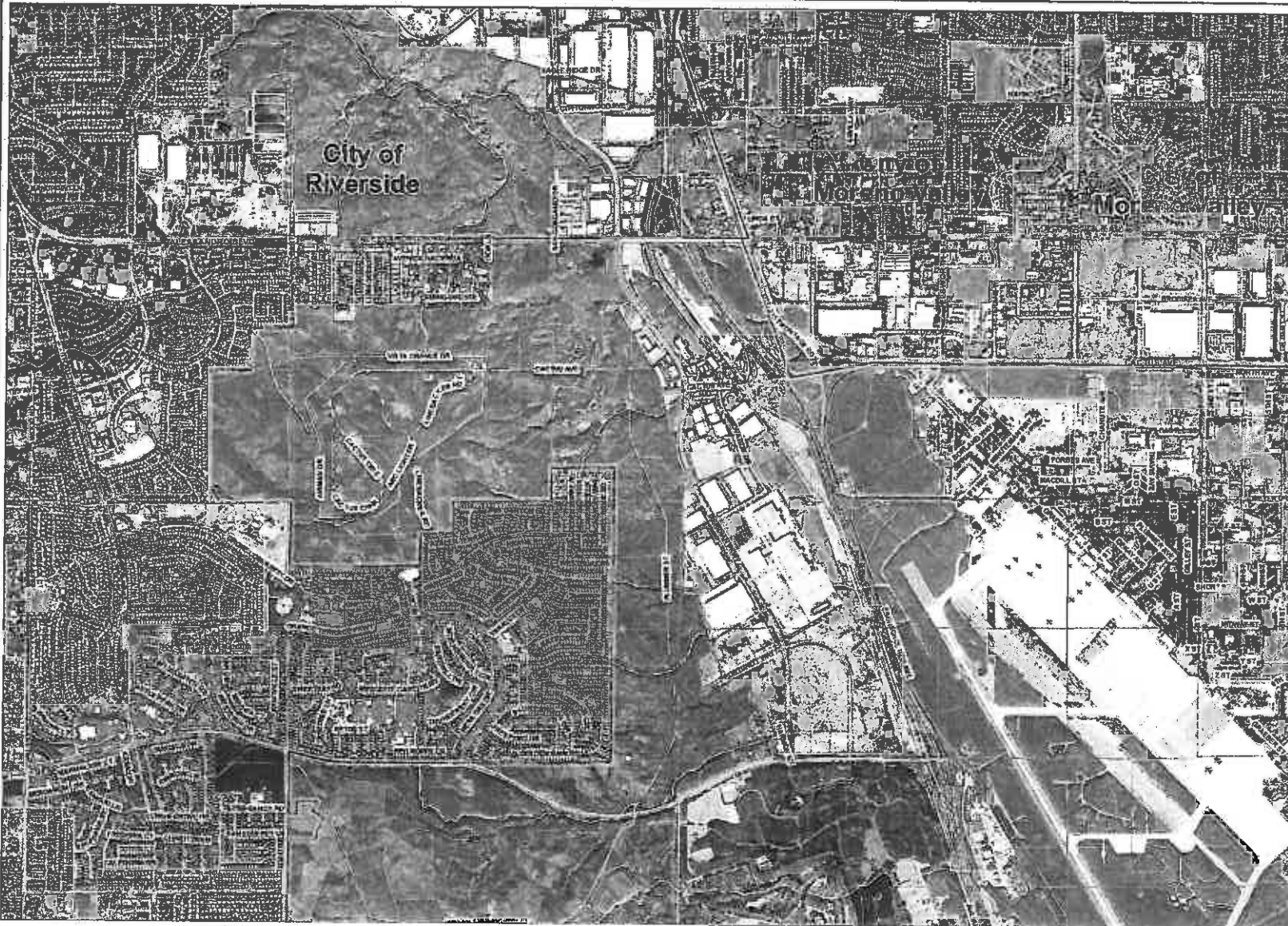
cc: Greens Group/Greens Mission Grove, LLC (applicant/payee)
E & T Enterprises/Mission Grove Plaza, c/o Reg. Prop. Inc. (property owner)
Denise Hauser or Sonia Pierce, March Air Reserve Base
Gary Gosliga, Airport Manager, March Inland Port Airport Authority
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1139MA15\ZAP1139MA15.LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

My Map



Legend

- City Boundaries
- roadsanno
- highways
 - HWY
 - INTERCHANGE
 - INTERSTATE
 - OFFRAMP
 - ONRAMP
 - USHWY
- roads
 - Major Roads
 - Arterial
 - Collector
 - Residential
- counties
- cities
- hydrography
- lines
- waterbodies
 - Lakes
 - Rivers



0 3,780 7,560 Feet



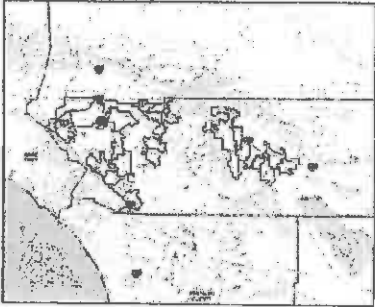
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON...8/31/2015 9:01:21 AM





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Notes

My Map



Legend

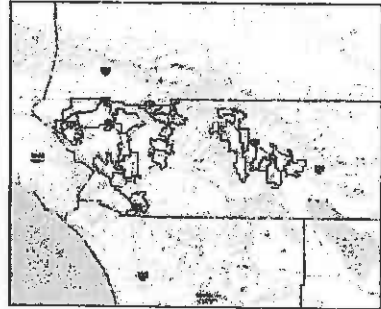
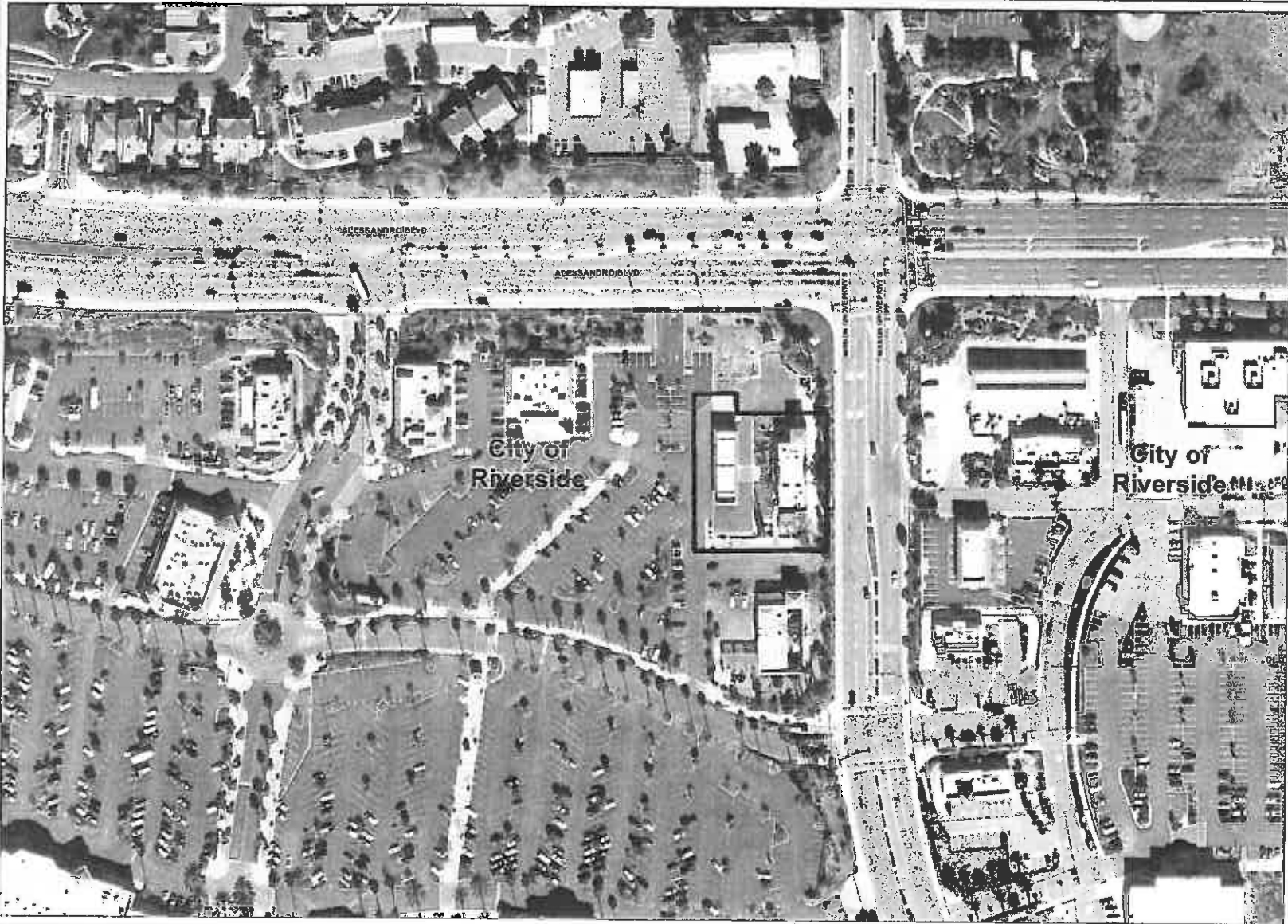
-  RCLIS Parcels
-  City Boundaries
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
-  counties
-  cities
- hydrographylines
- waterbodies
- Lakes
- Rivers

"IMPORTANT" Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

Notes



My Map



Legend

- RCLIS Parcels
- City Boundaries
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 236 473 Feet

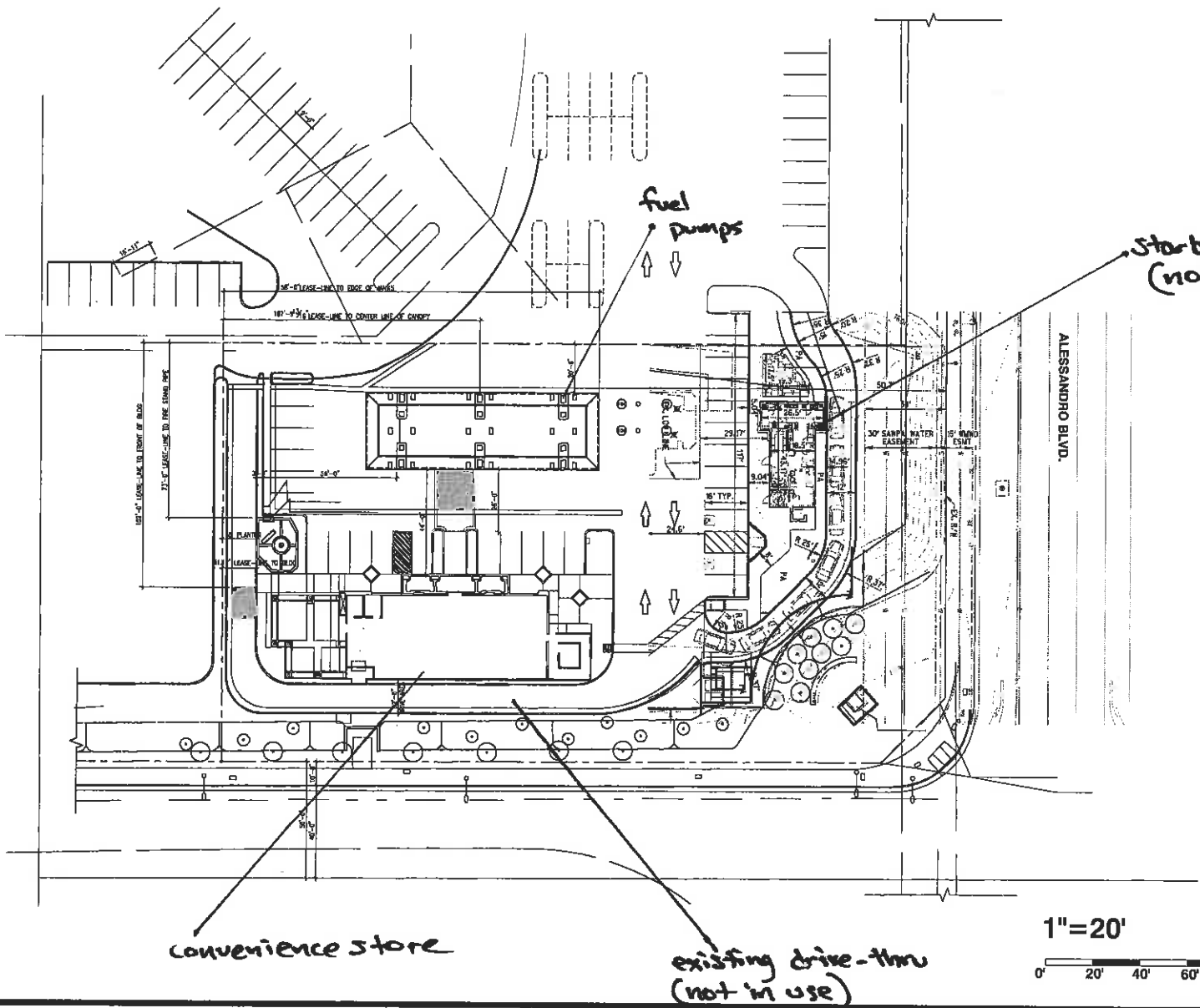


IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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Notes



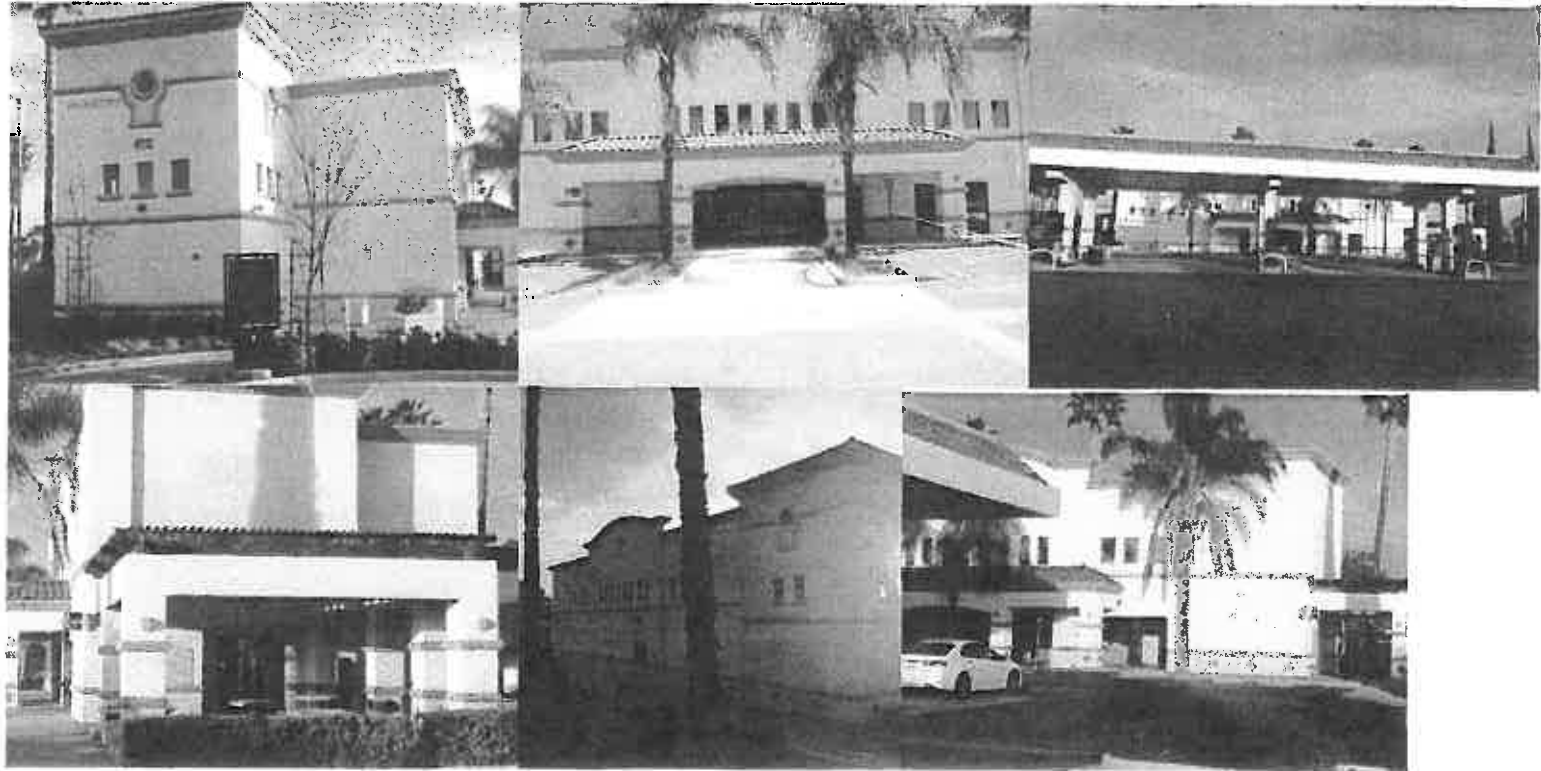
1"=20'
 0' 20' 40' 60' 80'

381 E ALESSANDRO
 RIVERSIDE, CALIFORNIA

GREENS MISSION GROVE LLC
 14252 CULVER DRIVE # A-358
 IRVINE, CA 92604

SITE PLAN
 4-15-15





**381 E ALESSANDRO
RIVERSIDE, CALIFORNIA**

GREENS MISSION GROVE, LLC
14252 CULVER DRIVE # A-358
IRVINE, CA 92604

ON SITE PHOTOS

4-15-15



GENERAL PROJECT NOTES

- A. ALL CONSTRUCTION SHALL COMPLY WITH APPLICABLE BUILDING CODES AND LOCAL RESTRICTIONS. CONTRACTORS MUST COMPLY WITH CONTRACTOR REGISTRATION REQUIREMENTS OF ALL GOVERNING AUTHORITIES. THE GENERAL REGISTRATION FEE SHALL BE PAID FOR BY THE OWNER. ALL OTHER FEES SHALL BE SECURED AND PAID FOR BY THE SUBCONTRACTOR DIRECTLY. RESPONSIBILITIES FOR ALL REQUIRED COUNTY AND/OR STATE LICENSES SHALL BE SECURED AND PAID FOR BY THE CONTRACTOR.
- B. APPROVED PLANS SHALL BE KEPT IN A PLAN BOX AND SHALL NOT BE USED BY ANYONE. ALL CONSTRUCTION SETS SHALL REFLECT SAME INFORMATION. CONTRACTOR SHALL MAINTAIN ONE COMPLETE SET OF PLANS ON THE PREMISES IN GOOD CONDITION AT ALL TIMES. THIS SHALL INCLUDE ALL ADDENDA AND CHANGE ORDERS.
- C. DISCREPANCIES BETWEEN PORTIONS OF THE CONTRACT DOCUMENTS, DRAWINGS AND SPECIFICATIONS ARE NOT INTENDED. THE CONTRACTOR IS TO CLARIFY ANY SUCH DISCREPANCIES WITH THE DESIGNER / CONSULTANT PRIOR TO COMMENCING WORK.
- D. STATED DIMENSIONS TAKE PRECEDENCE OVER GRAPHICS. DO NOT SCALE DRAWINGS TO DETERMINE LOCATIONS. NOTIFY THE DESIGNER / CONSULTANT OF ANY SUCH DISCREPANCIES PRIOR TO CONTINUING WITH WORK.
- E. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO LOCATE ALL EXISTING UTILITIES AND TO PROTECT THEM FROM DAMAGE. CONTRACTOR SHALL BEAR THE EXPENSE OF REPAIR OR REPLACEMENT OF UTILITIES OR OTHER PROPERTY DAMAGED BY OPERATIONS IN CONNECTION WITH THE EXECUTION OF THE WORK.
- F. ALL UTILITIES, OTHER THAN THE EXISTING GAS PIPING, REQUIRED BY CODES IN ACCORDANCE WITH NFPA 95 AND SHOWN HEREIN, FIRE SPRINKLER CONTRACTOR IS TO OBTAIN COMPLETE SHOP DRAWINGS, LAYOUT AND RELATED DATA TO BUILDING DEPARTMENT AND THE NEARBY FIRE DEPARTMENT PRIOR TO INSTALLATION.
- G. FOR CONSTRUCTION DETAILS NOT SHOWN, USE THE MANUFACTURER'S APPROVED SHOP DRAWINGS/DATA SHEETS IN ACCORDANCE WITH THE PROJECT REQUIREMENTS.
- H. THE CONTRACTOR SHALL VERIFY LOCATIONS OF ALL FOOD SERVICE EQUIPMENT AND COORDINATE WITH THE MECHANICAL, ELECTRICAL AND PLUMBING TRADES, AND ALL OTHER WORK UNDER THIS SCOPE OF WORK.
- I. ALL DETAILS AND SECTIONS SHOWN ON THE DRAWINGS ARE INTENDED TO BE TYPICAL AND SHALL BE CONSIDERED TO APPLY TO ANY SIMILAR SITUATION UNLESS NOTED OTHERWISE BY THE CONTRACTOR.
- J. IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO DETERMINE PRECISE LOCATION AND ELEVATION TO INSURE THE SAFETY OF THE BUILDING AND ITS PARTS DURING CONSTRUCTION.
- K. MATERIALS LISTED IN DRAWINGS ARE BASED ON DESIGN INTENT. ALTERNATE SPECIFICATIONS MAY BE ACCEPTED PROVIDED THEY GLOBALLY MATCH GOVERNMENT SPECIFICATIONS. CONTRACTOR IS TO SUBMIT PROPOSED SAMPLES OF SUBSTITUTIONS ALONG WITH SAMPLE OF THAT SPECIFIED IN DRAWINGS FOR REVIEW BY THE DESIGNER / CONSULTANT PRIOR TO BEING USED FOR CONSTRUCTION.
- L. TO REPAIR TO HIS DOCUMENT FACETS FOR OWNER'S SCOPE OF WORK, SAID DOCUMENT TAKES PRECEDENCE OVER ANY SCOPE THAT MAY BE PRESENTED IN THIS SET OF CONSTRUCTION DOCUMENTS OR SPECIFICATIONS.

VICINITY MAP



APPLICABLE CODES

- THE FOLLOWING BUILDING CODES ARE APPLICABLE:
- 2015 CALIFORNIA BUILDING CODE
 - 2015 CALIFORNIA PLUMBING CODE
 - 2015 CALIFORNIA MECHANICAL CODE
 - 2015 CALIFORNIA ELECTRICAL CODE
 - 2015 CALIFORNIA GREEN BUILDING STANDARDS CODE
 - 2015 CALIFORNIA FIRE CODE
 - 2015 CALIFORNIA ENERGY STANDARD
- REFERENCE MODEL CODES:
- 2012 INTERNATIONAL BUILDING CODE (IBC)
 - 2012 INTERNATIONAL MECHANICAL CODE (IMC)
 - 2012 INTERNATIONAL PLUMBING CODE (IPC)
 - 2012 NATIONAL ELECTRICAL CODE (NEC)

WITH CITY OF RIVERSIDE

DEFERRED SUBMITTALS

- SUBMITTAL DOCUMENTS FOR DEFERRED SUBMITTAL ITEMS SHALL BE SUBMITTED TO THE DESIGNER OR ENGINEER OF RECORD, WHO SHALL REVIEW THEM AND FORWARD THEM TO THE BUILDING OFFICIAL WITH A NOTATION INDICATING THAT THE DEFERRED SUBMITTAL DOCUMENTS HAVE BEEN REVIEWED AND THAT THEY HAVE BEEN FOUND TO BE IN GENERAL CONFORMANCE WITH THE DESIGN OF THE BUILDING. THE DEFERRED SUBMITTAL ITEMS SHALL NOT BE INSTALLED UNTIL THEIR DESIGN AND SUBMITTAL DOCUMENTS HAVE BEEN APPROVED BY THE BUILDING OFFICIAL. SEE 03.04.02 DEFERRED SUBMITTAL LIST (CHECK THAT APPLY).

- FIRE ALARM PROTECTION
- UNDERGROUND STORAGE TANKS
- FIRE ALARM SYSTEM
- SIGNAGE
- LANDSCAPE
- TRASH ENCLOSURE

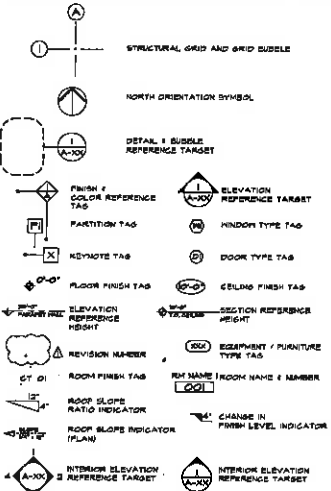
PERMITS & APPROVALS

- APPROVALS REQUIRED FOR THIS PROJECT:
- APPROVAL IS REQUIRED FROM THE CITY PLANNING DEPARTMENT PRIOR TO ISSUANCE OF THE BUILDING PERMIT, LANDSCAPE & IRRIGATION APPROVAL IS REQUIRED FROM THE CITY COMMUNITY DEVELOPMENT DEPARTMENT PRIOR TO ISSUANCE OF THE BUILDING PERMIT.
 - APPROVAL IS REQUIRED FROM THE CITY PUBLIC WORKS DEPARTMENT PRIOR TO ISSUANCE OF THE BUILDING PERMIT.
 - APPROVAL IS REQUIRED FROM THE CITY FIRE DEPARTMENT PRIOR TO ISSUANCE OF THE BUILDING PERMIT.
 - ALL SIGNAGE REQUIRES A SEPARATE APPROVAL AND A SEPARATE PERMIT.

ASSESSOR'S PARCEL NUMBER

278110002 (ZONE: CR-SP);
225350029 (ZONE: CG)
& 225350006 (ZONE: CG)

ARCHITECTURAL SYMBOLS



PROPOSED INTERIOR REMODEL TO EXISTING C-STORE

381 ALLESANDRO BLVD, RIVERSIDE CA 92508

PROJECT TEAM

OWNER/CLIENT INFORMATION

THE GREEN GROUP
881 ALLESANDRO BLVD
RIVERSIDE CA 92508
TEL (951) 528-5485
FAX (951) 528-5485
EMAIL info@thegreengroup.com

ARCHITECTURAL DESIGN

WESTERN STATES ENGINEERING, INC.
4887 E. LA PALMA AVE. SUITE 101
ANAHEIM, CA 92805
TEL (714) 945-4300
FAX (714) 945-1002
EMAIL info@westernstates.com / info@wse.com
CONTACT: JOSEPH KARAOKI / KELVIN LE

EMP DESIGN

GIANCARLO MORAL PE
439 E OCEAN BLVD.
LONG BEACH CA 90802
TEL (562) 504-5888
FAX (562) 504-5888
EMAIL gmoral@gsa.com
CONTACT: GIANCARLO MORAL PE

PROJECT DIRECTORY

PLANNING

PLANNING DIVISION
3800 MAIN STREET, 4TH FLOOR
RIVERSIDE, CA 92503
TEL (951) 528-5871
FAX (951) 528-5868
EMAIL info@westernstates.com
CONTACT: PATRICIA BRIDES

BUILDING DIVISION
3800 MAIN STREET, 4TH FLOOR
RIVERSIDE, CA 92503
TEL (951) 528-5885
FAX (951) 528-5833
EMAIL info@westernstates.com
CONTACT: NON-STRUCTURAL PLAN REVISION/ANALYSIS C.B. DRACARY
SEISMIC PLAN DRAWER
EMAIL cbdracary@westernstates.com
TEL (951) 528-5806

PUBLIC WORKS

ENGINEERING DIVISION
3800 MAIN STREET, 4TH FLOOR
RIVERSIDE, CA 92503
TEL (951) 528-5844
FAX (951) 528-5844
CONTACT: DAVE LESH - DIVISION CHIEF

FIRE DEPARTMENT

CITY OF RIVERSIDE FIRE DEPT.
3005 ST. LAWRENCE STREET
RIVERSIDE, CA 92504
TEL (951) 528-5421
FAX (951) 528-5421
EMAIL info@cityofriverside.org
CONTACT: DAVE LESH - DIVISION CHIEF

POWER

RIVERSIDE PUBLIC UTILITIES
8401 ORANGE STREET
RIVERSIDE, CA 92501
TEL (951) 528-5449
FAX (951) 528-5449
EMAIL info@riversidepu.com
CONTACT: JIMMY BROWN

GAS

SOCAL GAS COMPANY
CENTRALIZED CORRESPONDENCE
P.O. BOX 350 SAN DIMAS CA 91770
TEL (909) 427-2000
FAX (909) 427-2000
EMAIL info@socalgas.com
CONTACT: JIMMY BROWN

WATER & SEWER

RIVERSIDE PUBLIC UTILITIES
8401 ORANGE STREET
RIVERSIDE, CA 92501
TEL (951) 528-5288 WATER
FAX (951) 528-5288
EMAIL info@riversidepu.com
CONTACT: JIMMY BROWN

TELEPHONE/ CABLE

AT&T
9911 ORANGE AVE. SUITE 100
RIVERSIDE, CA 92501
TEL (951) 278-5801
FAX (951) 278-5801
EMAIL info@att.net
CONTACT: JIMMY BROWN

PROJECT DATA

ZONING	CR-SP, DD
LAND USE	
AREA OF SITE	
C-STORE SETBACKS	REQUIRED
WEST FRONT	44'-0"
EAST REAR	24'-0"
NORTH (RIBS)	148'-10"
SOUTH (RIBS)	20'-10"
/SEE PARKING COMPUTATION BELOW/	
BUILDING DATA	
(E) C-STORE	
(E) FIRST FLOOR	8,400 S.F.
(E) TENANT IMPROVEMENT + 3,475 S.F.	
(E) MEZZANINE FLOOR	445 S.F.
OCCUPANCY	1
TYPE OF COMB.	V-B
NUMBER OF STORY	2
BLDG. HEIGHT	28'-4"
(E) CANNOPY	
AREA	5,024 S.F.
OCCUPANCY	1
TYPE OF COMB.	V-B
NUMBER OF STORY	1
BLDG HEIGHT	20'-6"

PARKING COMPUTATION		
PARKING STANDARDS		
TYPE	SIZE	(E) PROVIDED
REGULAR	9'-0" X 18'-0"	4'-0" X 18'-0"
HANDICAP	9'-0" X 18'-0"	4'-0" X 18'-0"
LOADING	10'-0" X 22'-0"	8'-0" X 22'-0"
PARKING PROVIDED		
STANDARD PARKING	(E) PROVIDED	20
ACCESSIBLE PARKING	(E) PROVIDED	4
HANDICAP PARKING REQUIREMENTS		
FORMULA	REQUIRED	(E) PROVIDED
1 SPACE / 1-29 PARKING SPACES	(U)	(U)
CLEAN AIR VEHICLE PARKING		
FORMULA	REQUIRED	(E) PROVIDED
1 SPACE / 17-25 PARKING SPACES	(U)	(U)

SHEET INDEX

SHEET NO.	DESCRIPTION
GENERAL	
0-T-1	COVER SHEET
ARCHITECTURAL	
0-A-1.0	SITE PLAN (EXISTING)
0-A-1.1	EXISTING FLOOR PLAN
0-A-1.2	EXISTING MEZZANINE FLOOR PLAN
0-A-1.3	DRAC PLAN
0-A-1.0	FLOOR PLAN
0-A-2.0	ROOF PLAN
0-A-3.0	BUILDING ELEVATIONS
0-A-4.0	NOT USED
0-A-5.0	REFLECTED CEILING PLAN
0-A-5.1	CEILING DETAILS
0-A-5.2	INTERIOR ELEVATIONS
0-A-7.0	FINISHES PLAN & SCHEDULE
0-A-1.1	FINISH DETAILS
0-A-2.0	NOT USED
0-A-4.0	EQUIPMENT LAYOUT & SCHEDULE
PLUMBING	
P-0-1	PLUMBING SITE PLAN (EXISTING)
P-1-0	PLUMBING LAYOUT PLAN - HOT & COLD WATER
P-2-0	PLUMBING LAYOUT PLAN - HOT & WASTE / VENT
ELECTRICAL	
E-1-0	ELECTRICAL POWER PLAN
E-2-0	ELECTRICAL LIGHTING PLAN
E-12-1	TITLE 24
MECHANICAL	
M-1-0	MECHANICAL FLOOR HVAC PLAN
M-2-0	MECHANICAL DETAILS



WESTERN STATES ENGINEERING, INC.
 PROPOSED INTERIOR REMODEL TO EXISTING C-STORE
 381 ALLESANDRO BLVD, RIVERSIDE CA 92508



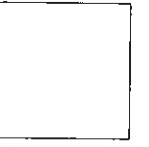
SHEET NUMBER
T-1.0
 JOB NO. 251415
 DATE 09/15/15
 CLIP #
 DR #



Western States Engineering, Inc.
 A PROFESSIONAL CORPORATION
 1887 E. LA PALMA BLVD., SUITE 100
 ANAHEIM, CALIFORNIA 92805
 TEL: (714) 933-8300 FAX: (714) 933-1000
 WWW.WESTERNSATES.COM

COMMITTEE SHEET

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OWNER NAME & ADDRESS
THE GREEN GROUP
 211 ALESSANDRO BLVD
 RIVERSIDE, CA

PROJECT NAME & ADDRESS
INTERIOR REMODEL TO EXISTING C-STORE
 211 ALESSANDRO BLVD
 RIVERSIDE, CA

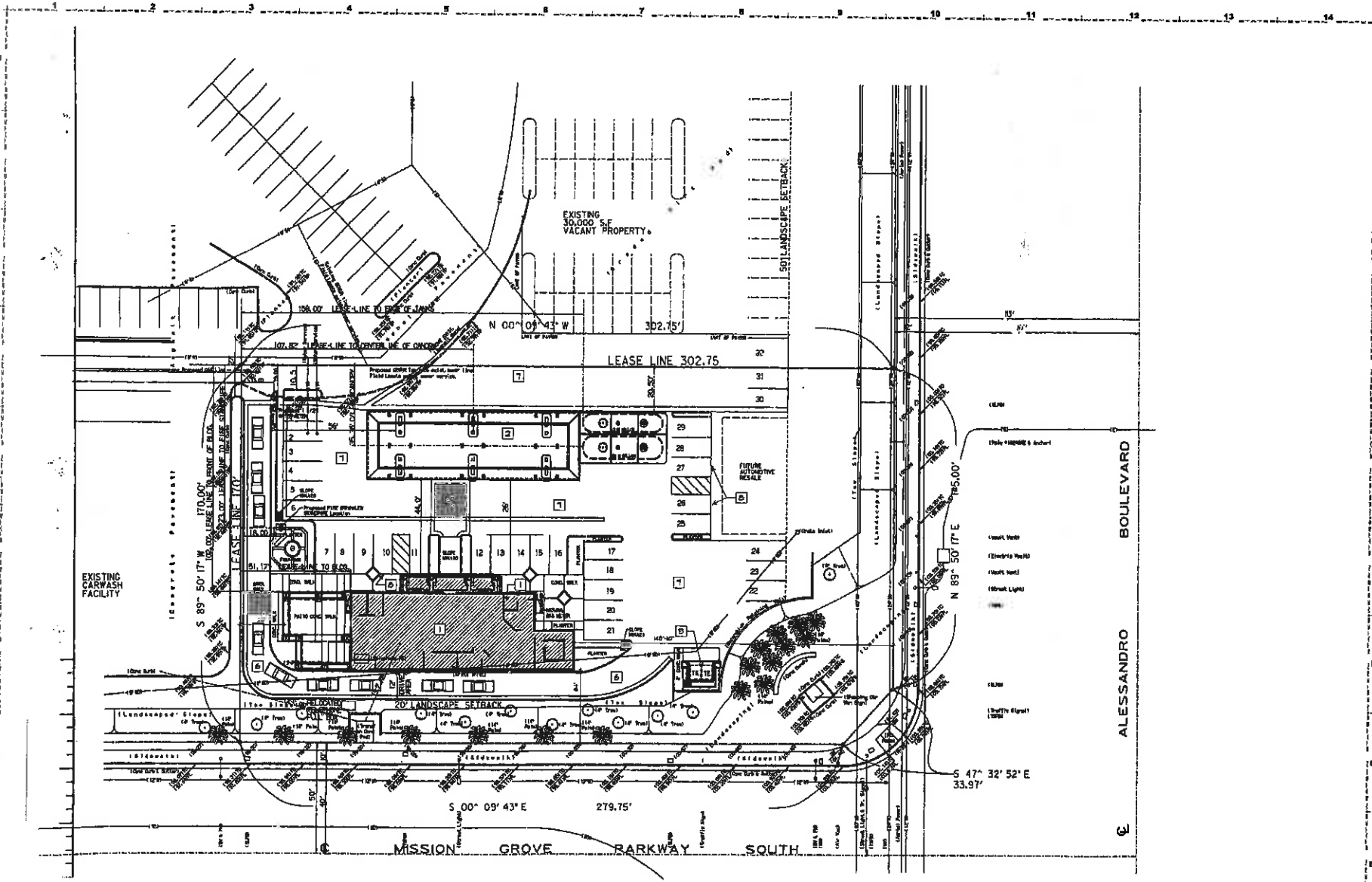
NOTE TO CONTRACTOR
 CONTRACTOR TO VERIFY ALL UTILITIES AND REMOVE THEM TO THE STREET.

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DESIGNED BY: VWS
 DRAWN BY: VWS
 CHECKED BY: JK
 DATE DRAWN: 8-2-15
 SCALE: 1"=20'-0"

EXISTING SITE PLAN

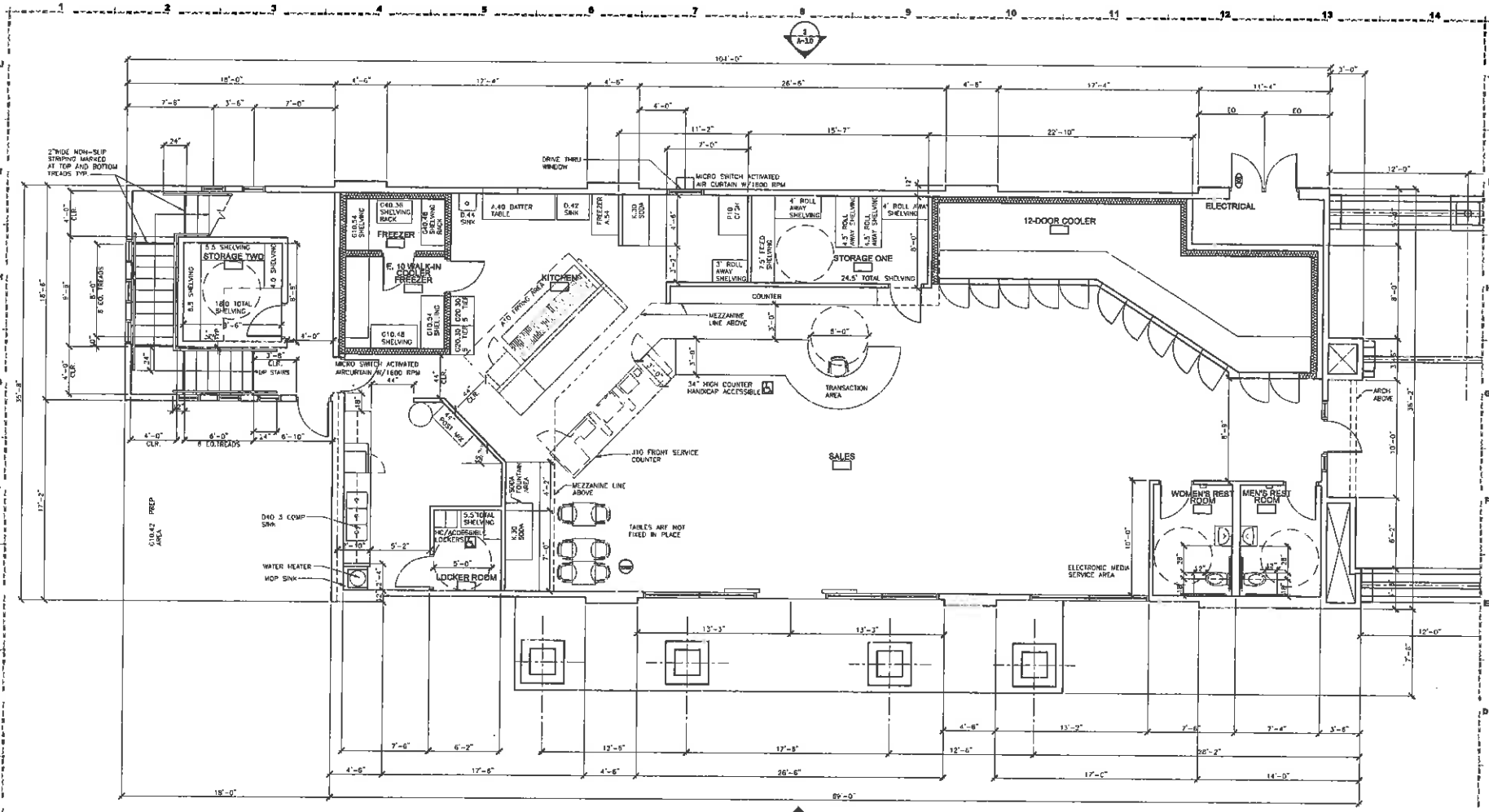
SHEET NUMBER
AS-1.0
 JOB No. E91415
 CUP No.



1 SITE PLAN (EXISTING)
 SCALE: 1" = 20'-0"



CONSULTANT'S SEAL



REVISIONS

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OWNER NAME & ADDRESS
 THE GREEN GROUP
 381 ALESSANDRO BLVD
 SAN DIEGO, CA

PROJECT NAME & ADDRESS
 76 STATION
 381 ALESSANDRO BLVD
 SAN DIEGO, CA

NOTE TO CONTRACTOR
 CONSULT TO SPEC 10000 FOR DETAILS OF ALL ELECTRICAL WORK AND TO SPEC 21000 FOR DETAILS OF PLUMBING.

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DESIGNED BY: WJ
DRAWN BY: SA
CHECKED BY: JK
DATE DRAWN: 02.28.11
SCALE: 1/8"=1'-0"
PROJECT TITLE:
 EXISTING FLOOR PLAN

SHEET NUMBER
A-0.1
JOB NO: E01415
CUP NO.:

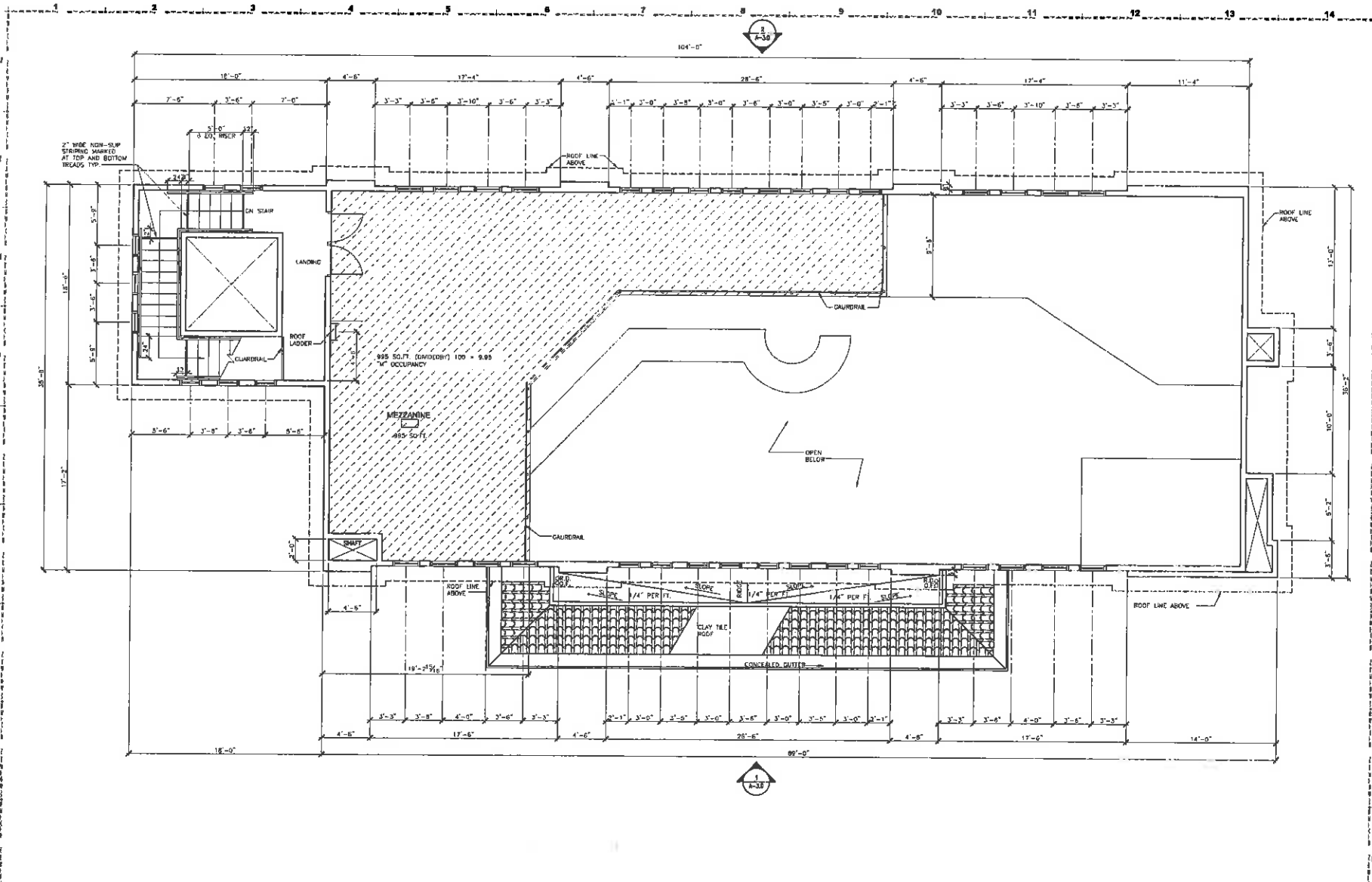
1 EXISTING FLOOR PLAN
 SCALE: 1/8" = 1'-0"

ALL DIMENSIONS UNLESS OTHERWISE SPECIFIED ARE IN FEET AND INCHES. DIMENSIONS IN PARENTHESES ARE FOR INFORMATION ONLY. DIMENSIONS IN FEET AND INCHES SHALL BE USED FOR CONSTRUCTION. DIMENSIONS IN METERS ARE FOR INFORMATION ONLY. DIMENSIONS IN METERS SHALL BE USED FOR CONSTRUCTION. DIMENSIONS IN FEET AND INCHES SHALL BE USED FOR CONSTRUCTION. DIMENSIONS IN METERS ARE FOR INFORMATION ONLY. DIMENSIONS IN METERS SHALL BE USED FOR CONSTRUCTION.



Western States Engineering, Inc.
 A PROFESSIONAL CORPORATION
 4807 E. LA PALMA STE. 207
 DENVER, CO 80231
 TEL: (773) 486-8411 FAX: (773) 486-1002
 WWW.WESTERNSTATES.COM

REG. CONSULTANT SEAL



REVISIONS

NO.	DESCRIPTION	DATE
1		
2		
3		

OWNER NAME & ADDRESS
THE GREEN GROUP
 381 ALESSANDRO BLVD
 BIRMINGHAM, GA

PROJECT NAME & ADDRESS
78 STATION
 381 ALESSANDRO BLVD
 BIRMINGHAM, GA

NOTE TO CONTRACTOR
 REFER TO ALL OTHER DRAWINGS FOR DETAILS OF ALL WORK TO BE DONE BY THE CONTRACTOR.

CONTRACTOR'S STATEMENT
 I, the undersigned, who am a duly licensed Professional Engineer in the State of Georgia, hereby certify that I am the author of the design and construction of the above described work, and that I am a duly licensed Professional Engineer in the State of Georgia. I am not aware of any other person who has been or will be engaged in the design and construction of the above described work, and I am not aware of any other person who has been or will be engaged in the design and construction of the above described work.

DESIGNED BY: WJ
 DRAWN BY: BK
 CHECKED BY: JC
 DATE DRAWN: 02.28.12
 SCALE: 1/4" = 1'-0"

EXISTING MEZZANINE FLOOR PLAN

PROJECT NUMBER
A-0.2
 JOB NO. E81415
 CUP NO. CUP NO.

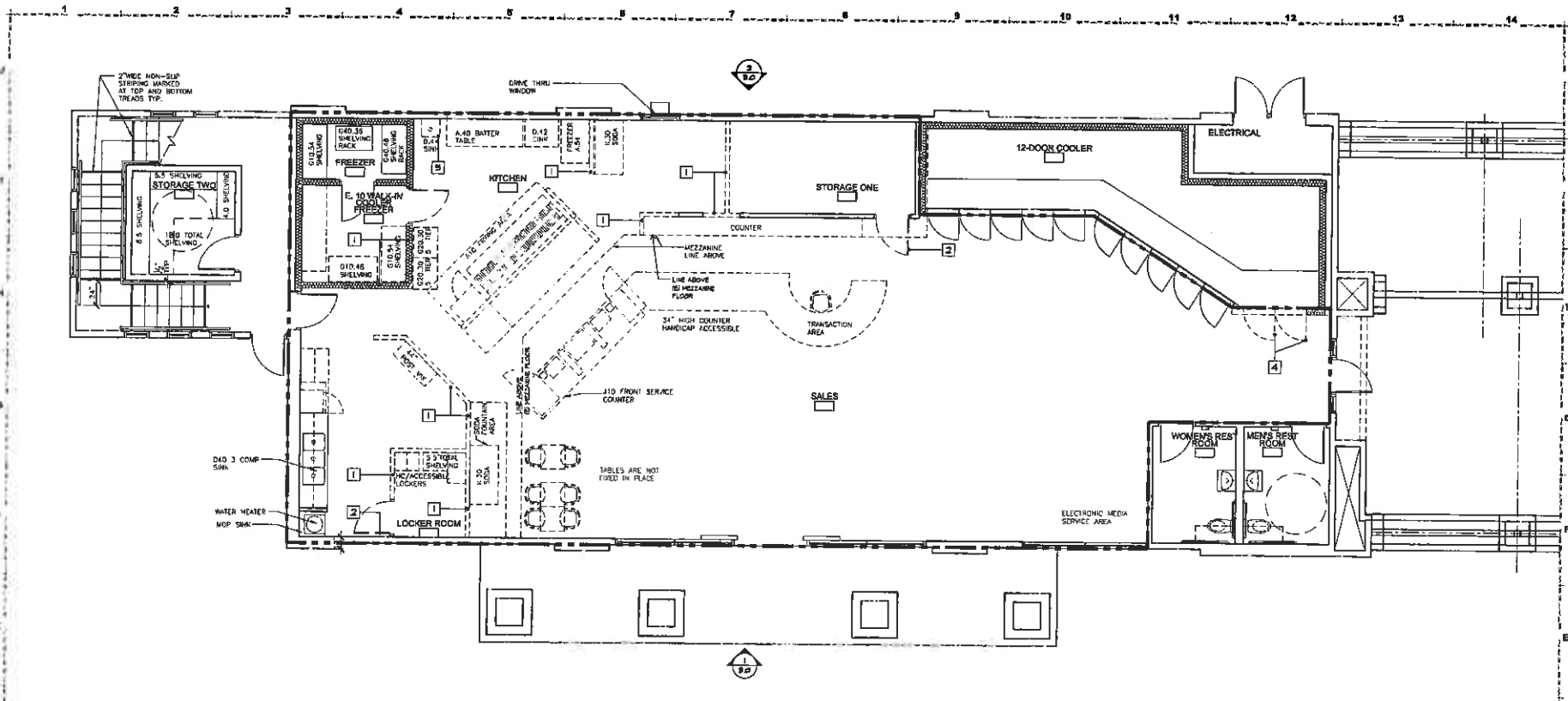
1 EXISTING MEZZANINE FLOOR PLAN
 SCALE: 1/4" = 1'-0"

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Western States Engineering, Inc.
 A PROFESSIONAL ENGINEERING FIRM
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 ANAHEIM, CALIFORNIA 92805
 TEL: (714) 938-0000 FAX: (714) 938-0001
 WWW.WESTERSTATES.COM

REGISTERED PROFESSIONAL ENGINEER
 NO. 45872
 STATE OF CALIFORNIA



1 DEMOLITION PLAN
 SCALE: 1/4" = 1'-0"
 NORTH

DEMOLITION KEY NOTES

- 1 REMOVE AND DISPOSE OF EXISTING WALL SECTION
- 2 REMOVE AND DISPOSE OF EXISTING DOORS
- 3 PROTECT IN PLACE EXISTING STEEL COLUMN SUPPORT
- 4 REMOVE EXISTING COOLER DOORS
- 5 REMOVE AND DISPOSE OF EXISTING SIGN AND LETTERS AS NOTED

WALL LEGEND

- (S) WALLS TO REMAIN
- (E) WALK-IN COOLER WALLS TO REMAIN
- INTERIOR WALLS TO BE DEMOLISHED
- OUTLINE OF TENANT IMPROVEMENT AREA

NOTES

- CONTRACTOR TO VERIFY THE BUILDING DIMENSIONS AND THE EXISTING STRUCTURAL ELEMENTS BEFORE STARTING THE CONSTRUCTION
- ALL EXISTING STRUCTURAL FRAMING TO REMAIN AS PER STRUCTURAL DRAWINGS DNO.
- ALL WALLS TO BE REMOVED ARE NOT BEAMS / SHEAR WALLS

- REVISIONS
- NO. DESCRIPTION BY DATE
- 1
- 2
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OWNER NAME & ADDRESS
THE GREEN GROUP

341 ALEXANDRO BLVD
 RIVERSIDE, CA

PROJECT NAME & ADDRESS
INTERIOR REMODEL TO EXISTING C-STORE
 341 ALEXANDRO BLVD
 RIVERSIDE, CA

NOTE TO CONTRACTOR
 REMOVE ALL LIGHT FIXTURES TO BE RE-INSTALLED BY OTHER TRADES

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DESIGNED BY: WS
 DRAWN BY: BA
 DATE DRAWN: 05/08/15
 SCALE: 1/4" = 1'-0"
FIRST FLOOR DEMOLITION PLAN

SHEET NUMBER
A-03
 JOB NO. E91415
 DRAW NO. CUP NO.



Western States Engineering, Inc.
 A PROFESSIONAL CORPORATION
 4647 W. LA PALMA BLVD., 707
 PALM BEACH, FL 33411
 TEL: (561) 832-1100 FAX: (561) 832-1101
 WWW.WESTERSTATS.COM

■ CONSULTANT SEAL

■ SUBMITTAL

NO. DESCRIPTION BY DATE
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■ REVISIONS

NO. DESCRIPTION BY DATE
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■ OWNER NAME & ADDRESS
 THE GREEN GROUP

311 ALABAMA BLVD
 RICHMOND, GA

■ PROJECT NAME & ADDRESS
 INTERIOR REMODEL
 TO EXISTING C-STORE

311 ALABAMA BLVD
 RICHMOND, GA

■ NOTE TO CONTRACTOR
 (INCLUDE IN ALL SUBMITTALS AND SPEC'S)
 1. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE BUILDING CODES AND ALL APPLICABLE LOCAL, STATE AND FEDERAL REGULATIONS.
 2. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE BUILDING CODES AND ALL APPLICABLE LOCAL, STATE AND FEDERAL REGULATIONS.
 3. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE BUILDING CODES AND ALL APPLICABLE LOCAL, STATE AND FEDERAL REGULATIONS.
 4. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE BUILDING CODES AND ALL APPLICABLE LOCAL, STATE AND FEDERAL REGULATIONS.

■ ENVIRONMENTAL STATEMENT
 USE GREEN AND SUSTAINABLE MATERIALS AND FINISHES TO REDUCE THE ENVIRONMENTAL IMPACT OF THIS PROJECT. SEE THE ENVIRONMENTAL STATEMENT FOR MORE INFORMATION.

■ DESIGNED BY: WS

■ DRAWN BY: KA

■ CHECKED BY: JF

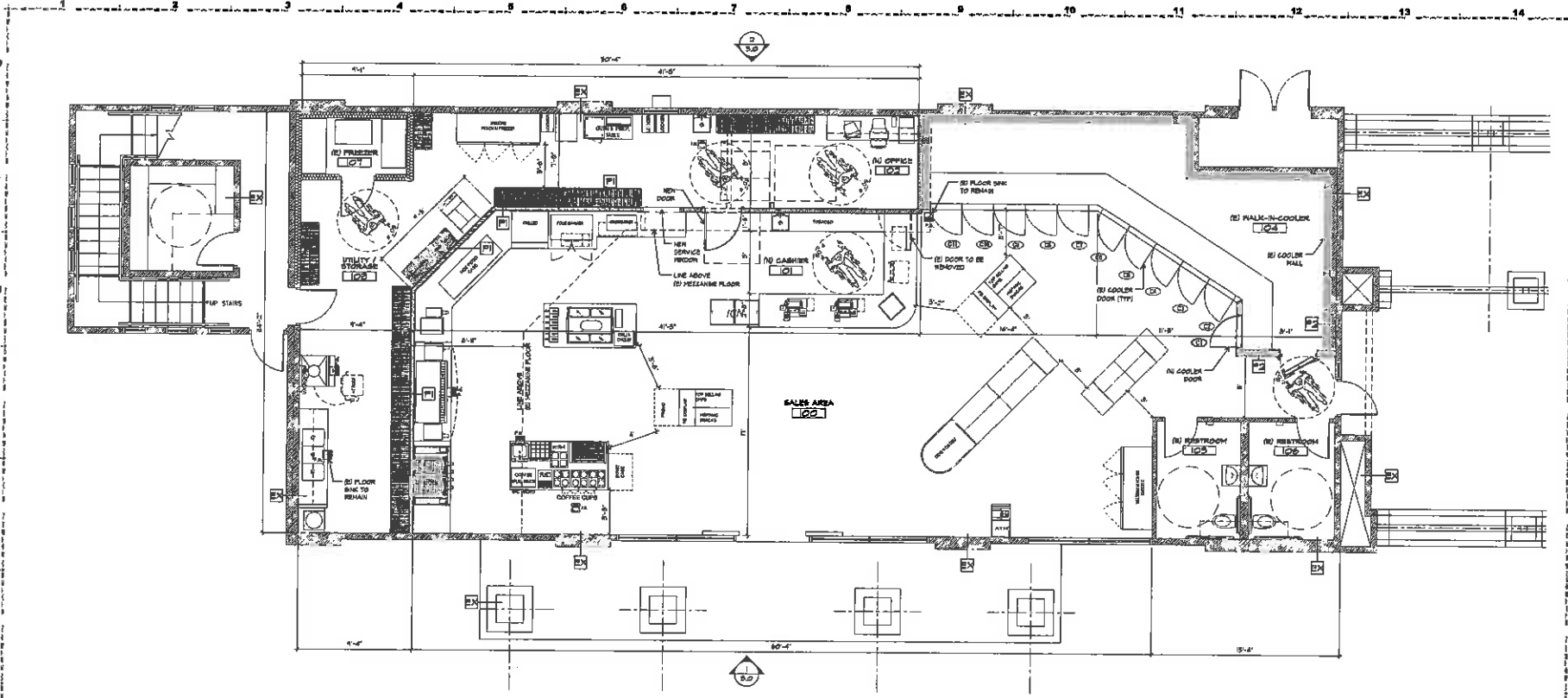
■ DATE DRAWN: 03.15.15

■ SCALE: 1/4" = 1'-0"

■ SHEET TITLE
 FIRST FLOOR PLAN

■ SHEET NUMBER

A-1.0
 CUP No.
 E91415 CUP No.



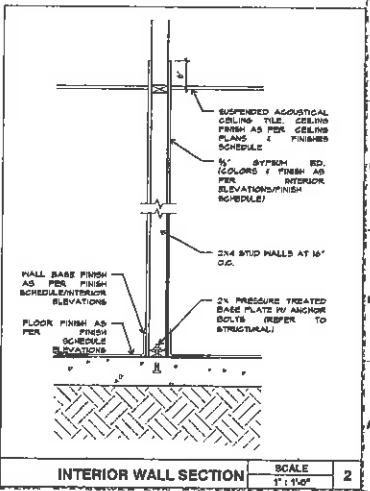
1 PROPOSED FLOOR PLAN
 SCALE: 1/4" = 1'-0"

WALL LEGEND

- 2" x 4" NEW INTERIOR STUD WALL @ 16" O.C. WITH 5/8" GYP SO ON EACH SIDE U.S.A. REFER TO DETAIL 2 ON SHEET A-1.0. USE MOISTURE RESISTANT GYPSUM BO. @ MET AREAS
- NEW WALK-IN COOLER WALLS AS PER MANUFACTURER'S SPECS. REFER TO DETAIL 1 ON SHEET A-1.0.
- EX (E) WALL TO REMAIN

NOTES

- FLOOR PLAN NOTES:**
- 1. ALL DRAWINGS ARE TO BE READ IN CONJUNCTION WITH STRUCTURAL, ELECTRICAL, MECHANICAL & PLUMBING DRAWINGS. COORDINATE ANY DISCREPANCIES WITH ARCHITECT/ENGINEER FOR CLARIFICATION BEFORE COMMENCING ANY WORK.
 - 2. ALL DIMENSIONS ARE TO FACE OF STUD UNLESS NOTED OTHERWISE.
 - 3. COORDINATE ALL DIMENSIONS AND DETAILS (NEW / EXISTING) WITH STRUCTURAL AND MEP DRAWINGS.
 - 4. PROVIDE & INSTALL ALL HOOD BLOCKS / PIERING STUDS REQUIRED TO PROVIDE ANCHORAGE FOR ALL FINISHES, ACCESSORIES, FIXTURES, ETC. TO COMPLETE ALL WORK.
 - 5. PROVIDE PORTABLE FIRE EXTINGUISHERS IN LOCATIONS AS REQUIRED BY FIRE CODE AND LOCAL FIRE AUTHORITY.

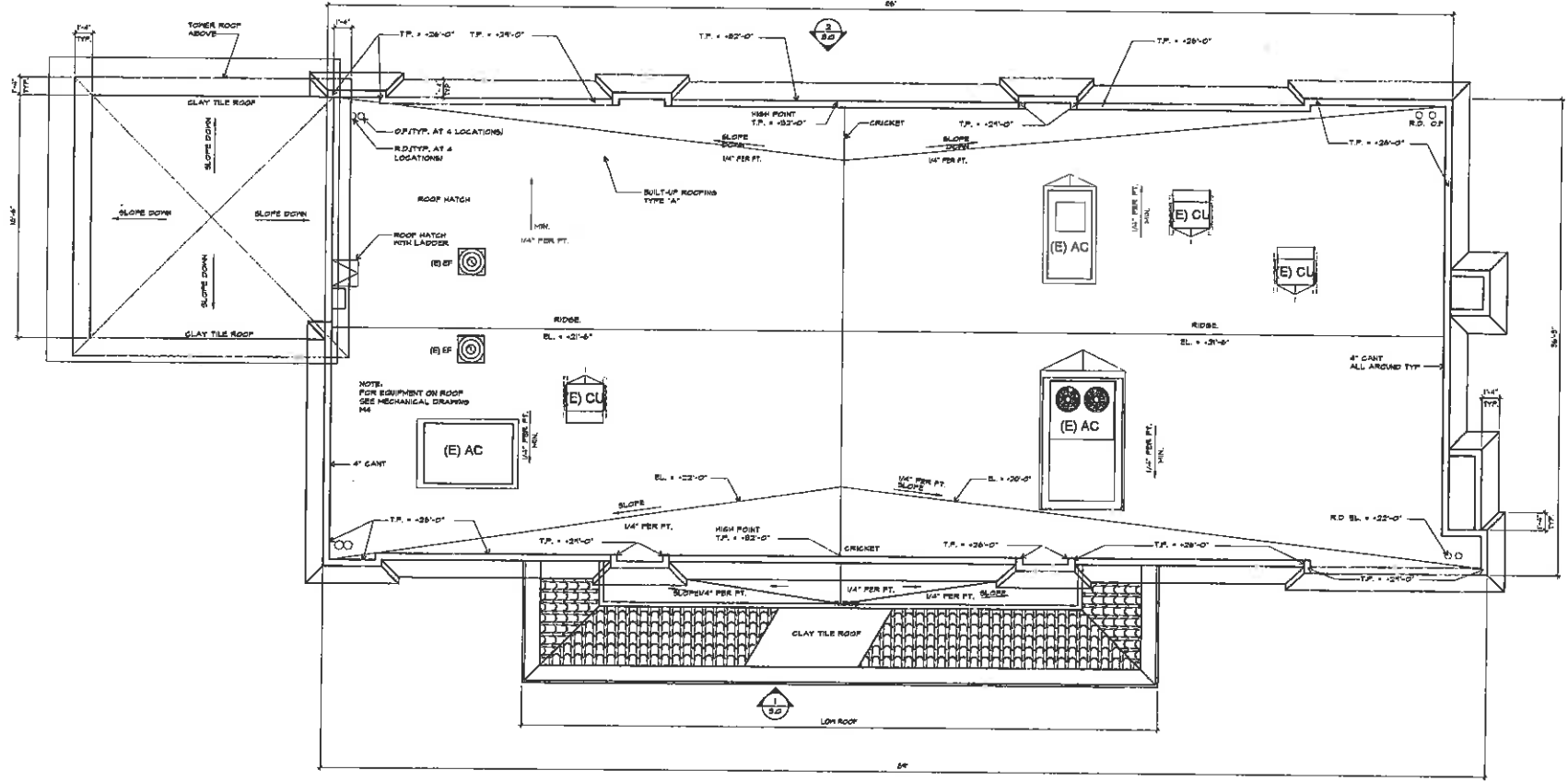


INTERIOR WALL SECTION
 SCALE: 1/4" = 1'-0" 2



Western States
Engineering, Inc.
A PROFESSIONAL CORPORATION
4887 E. LA PALMA AVE, 707
P.O. BOX 1000, 707
MOUNTAIN VIEW, CA 94039
WWW.WESTSTATES.COM

CONSULTANT SEAL



1 ROOF PLAN (EXISTING)
SCALE: 1/4" = 1'-0"

REVISIONS

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OWNER NAME & ADDRESS
THE GREEN GROUP
311 ALVARADO BLVD
MERCED, CA

PROJECT NAME & ADDRESS
INTERIOR REMODEL
TO EXISTING C-STORE
311 ALVARADO BLVD
MERCED, CA

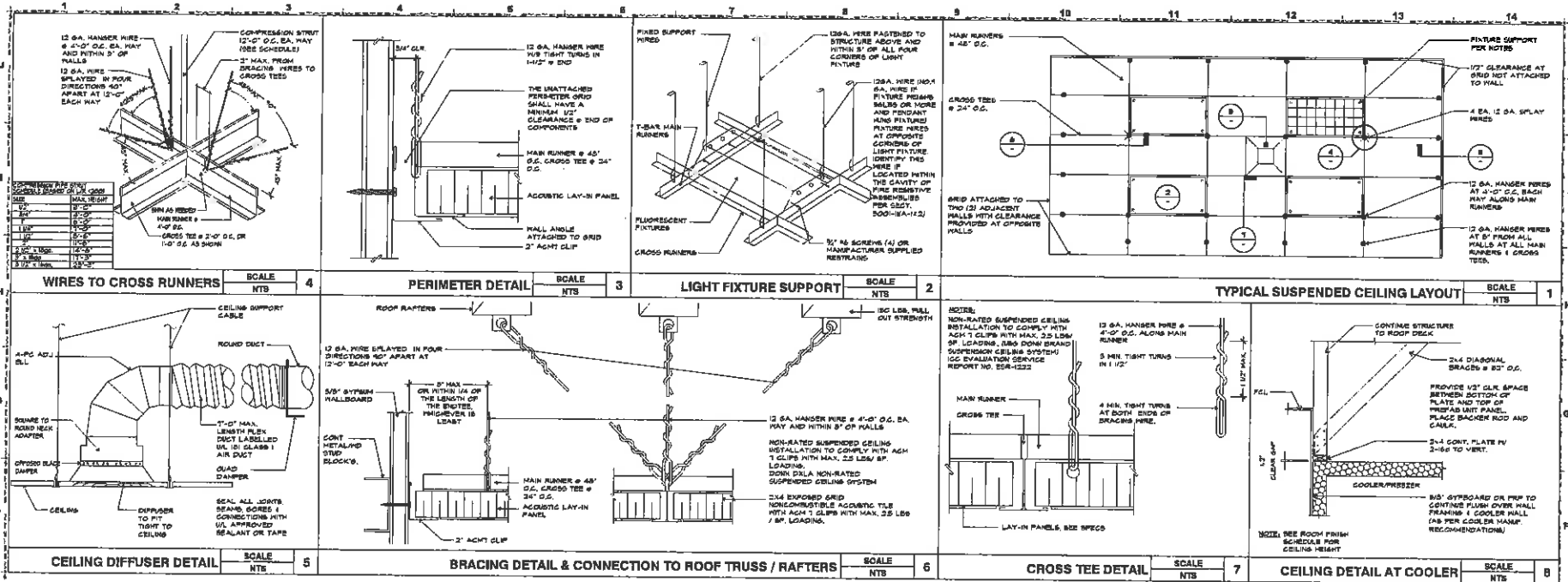
NOTE TO CONTRACTOR
REFER TO MECHANICAL DRAWING FOR
EQUIPMENT ON ROOF

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DESIGNED BY: WJ
DRAWN BY: WS
CHECKED BY: JK
DATE DRAWN: 05-01-15
SCALE: 1/4" = 1'-0"
SHEET TITLE

ROOF PLAN
(EXISTING)

SHEET NUMBER
A-2.0
JOB No. E91415
COPY No. CUP NO.



REVISION	NO.	DESCRIPTION	BY	DATE
1	1	ISSUED FOR PERMIT	JK	04/23/15
2	2			
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OWNER NAME & ADDRESS
THE GREEN GROUP
311 ALABAMA BLVD
RIVERSIDE, CA

PROJECT NAME & ADDRESS
INTERIOR REMODEL TO EXISTING C-STORE
311 ALABAMA BLVD
RIVERSIDE, CA

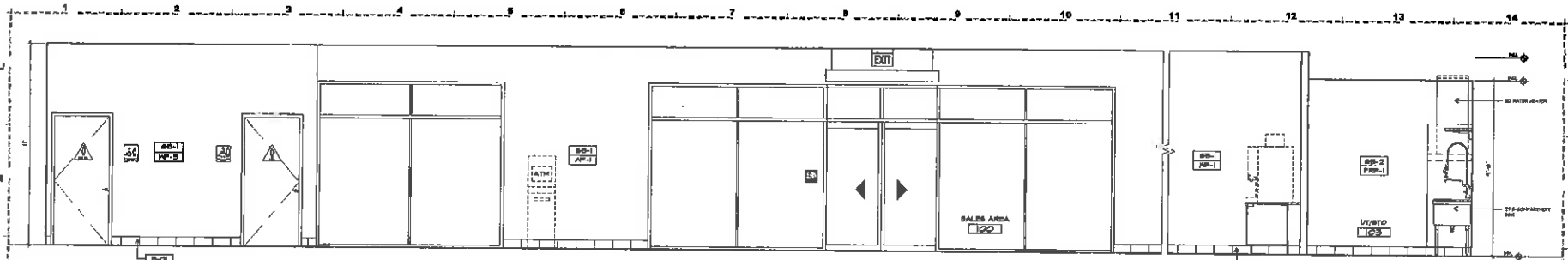
NOTE TO CONTRACTOR
CONTRACTOR TO VERIFY ALL CONDITIONS AND CONDITIONS OF WORK AS SHOWN ON THIS DRAWING AND AS NOTED BY THE ARCHITECT.

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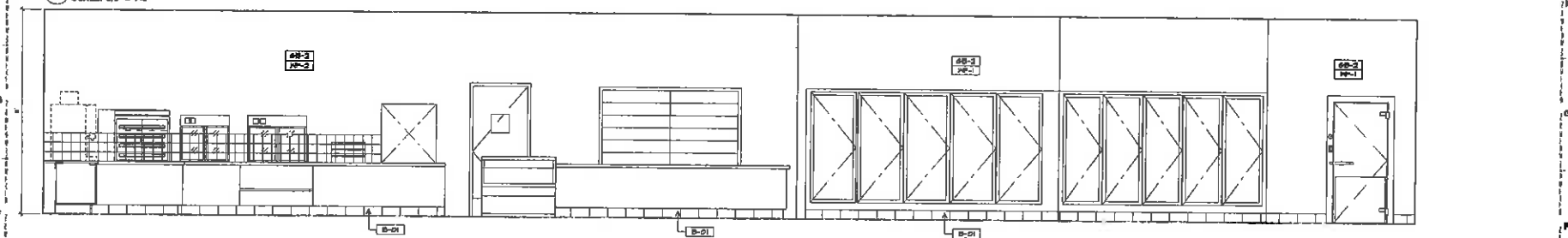
DESIGNED BY: JK
CHECKED BY: JK
DATE DRAWN: 04/23/15
SCALE: AS SHOWN
SHEET TITLE:

CEILING DETAILS

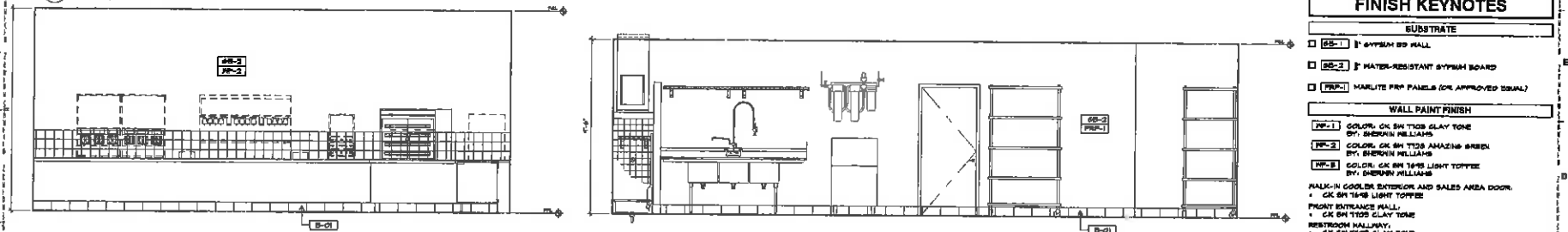
SHEET NUMBER
A-5.1
NO. OF SHEETS: 15
DATE: 04/14/15



1 INTERIOR ELEVATION- SALES AREA/MAIN ENTRANCE
SCALE: 3/8" = 1'-0"

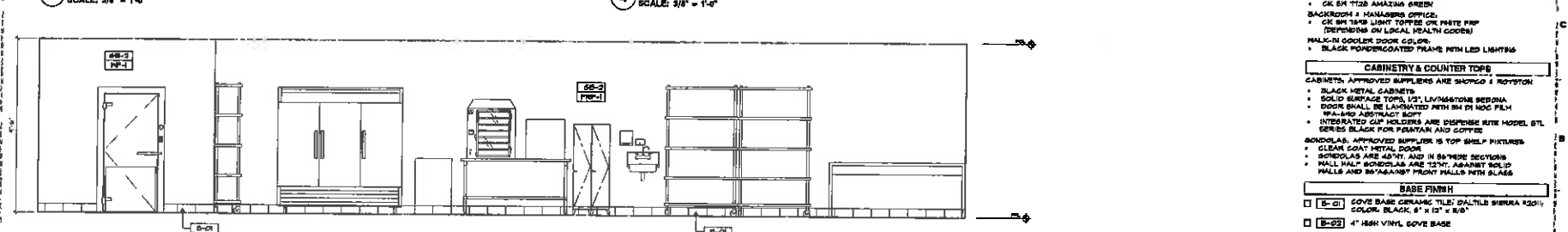


2 INTERIOR ELEVATION- CASHIER'S COUNTER & SALES AREA
SCALE: 3/8" = 1'-0"



3 INTERIOR ELEVATION- COFFEE/SODA
SCALE: 3/8" = 1'-0"

4 INTERIOR ELEVATION- UT/STORAGE
SCALE: 3/8" = 1'-0"



5 INTERIOR ELEVATION- UT/STORAGE
SCALE: 3/8" = 1'-0"

FINISH KEYNOTES

- | SUBSTRATE | |
|-----------|--|
| [SB-1] | GYPSUM BO WALL |
| [SB-2] | WATER-RESISTANT GYPSUM BOARD |
| [FRP-1] | MARLITE FRP PANELS (OR APPROVED EQUAL) |
-
- | WALL PAINT FINISH | |
|-------------------|---|
| [WP-1] | COLOR: GK 5H T102 SLAY TONE
BY: SHERWIN WILLIAMS |
| [WP-2] | COLOR: GK 5H T102 AMAZING GREEN
BY: SHERWIN WILLIAMS |
| [WP-3] | COLOR: GK 5H T102 LIGHT TOPPICE
BY: SHERWIN WILLIAMS |
-
- | CABINETRY & COUNTER TOPS | |
|--------------------------|--|
| [CB-1] | BLACK METAL CABINETRY |
| [CB-2] | SOLID SURFACE TOPS, 1/2" LAMINATION BEGONIA |
| [CB-3] | DOORS SHALL BE LAMINATED WITH 3/4" P1 HOG FILM |
| [CB-4] | WAL-BRO ABSTRACT BOY |
| [CB-5] | INTERGRADED CUP HOLDERS ARE DISPENSE MTR MODEL 87L |
| [CB-6] | SERIES BLACK FOR FRONT AND COFFICE |
-
- | BASE FINISH | |
|-------------|--|
| [B-1] | COVE BASE CERAMIC TILE (MULTI SERRA 1301)
COLOR: BLACK, 8" X 12" X 3/8" |
| [B-2] | 4" HIGH VINYL COVE BASE |

REVISIONS	NO.	DESCRIPTION	DATE
1			
2			
3			
4			

OWNER NAME & ADDRESS
THE GREEN GROUP
311 ALESSANDRO BLVD
REDSHORE, CA

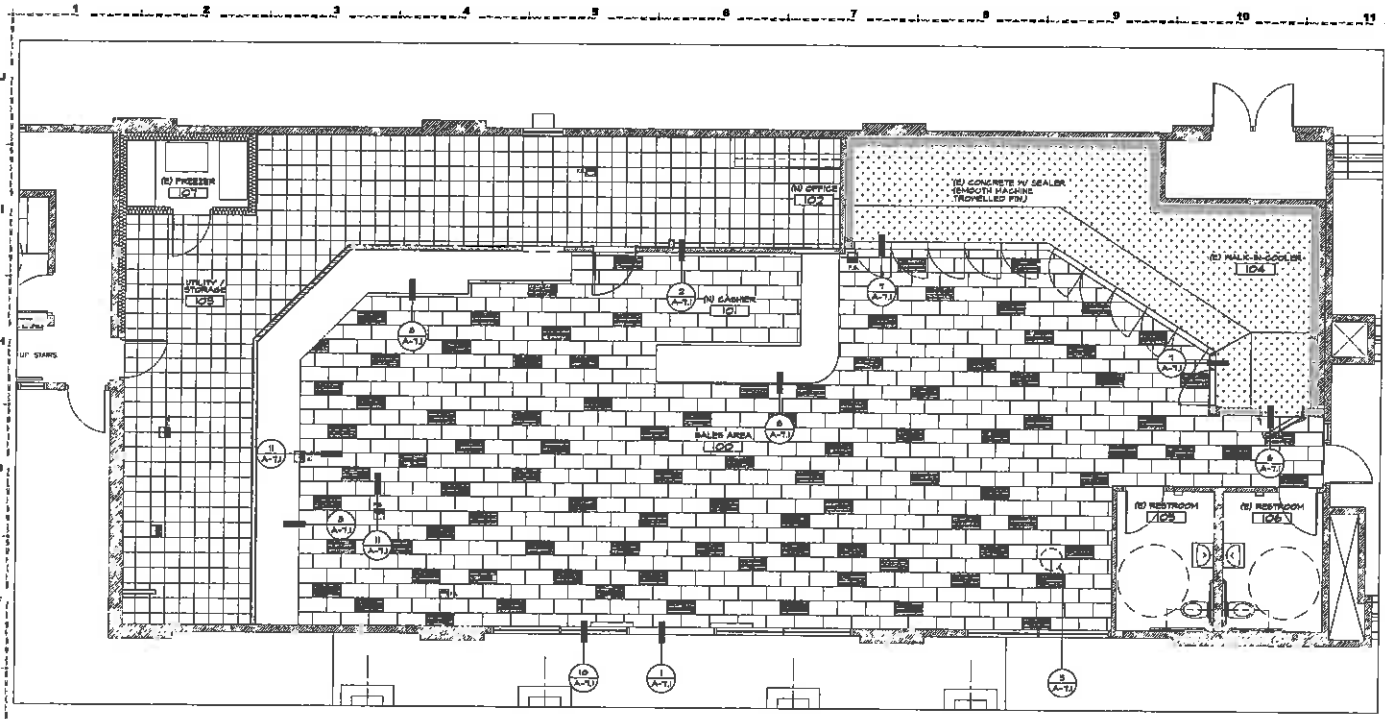
PROJECT NAME & ADDRESS
INTERIOR REMODEL TO EXISTING C-STORE
311 ALESSANDRO BLVD
REDSHORE, CA

NOTE TO CONTRACTOR
(INCLUDE IN EVERY LEAVE-IN SET, IN ALL WORK AREAS TO BE RE-OPENED TO THE CONTRACTOR)

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DESIGNED BY: WS
DRAWN BY: WS
CHECKED BY: JK
DATE DRAWN: 01.11.11
SCALE: AS SHOWN
ENTRY TITLE: INTERIOR ELEVATIONS

PROJECT NUMBER:
A-6.0
JOB NO.: **CUP NO.:**
E01415 CUP NO.



1 FLOOR FINISH PLAN
SCALE: 1/4" = 1'-0"
NORTH

FINISH SCHEDULE						
MARK	ROOM NAME	FLOOR	BASE	WALL	CEILING	REMARKS
100	SALES AREA	1-2	1-2	1-2	1-2	FORM TO THE BACK OF SODA FOUNTAIN AREAS BY FINI OR 3 LAYERS OF TILE
101	CASHIER	1-2	1-2	1-2	1-2	
102	OFFICE	1-2	1-2	1-2	1-2	
103	UTILITY STORAGE RM	1-2	1-2	1-2	1-2	
104	HALL-W-COOLER	1-2	1-2	1-2	1-2	ENCASED CONCRETE WITH RE-BETRE 120 SEALER BY PACIFIC POLYURETHANE INTL. OR APPROVED EQUAL.

LEGEND

- CROSSVILLE, COLORADO STONE SERIES 12" X 12" SQUARES 1/4" BENT BY 1/4" SANDSTONE GROUT (LATICRETE)
- 4" 12" X 24" X 5/16" BROWN LYBTRA REGAN PORCELAIN TILE WHITE FINISH, FLOOR/DECK, 5/8" (CROSSBRIE) RATIO, 80%
- NON SKID TILE, 12" X 12" X 5/16" IMPOLISHED TILES (COLOR B) ESCORT (SHEAR POINTMENT) (LATICRETE) USE SMOOTH TILE FILLER
- CONCRETE W/ SEALER (SMOOTH MACHINE TROWELLED FINI) TO MATCH EXISTING
- (R) CONCRETE W/ SEALER (SMOOTH MACHINE TROWELLED FINI)
- FLOOR BANK
- FLOOR DRAIN

FINISH NOTES:

1. ALL FINISHES SHALL COMPLY WITH TABLE 803.3, CBC
2. TRANSITIONS FROM ONE ROOM TO ANOTHER SHALL BE DONE UNDER CENTER OF DOORS WHERE POSSIBLE
3. COORDINATE FINAL LOCATION OF FD & FS WITH PLUMBING DRAININGS AND EQUIPMENT LAYOUT
4. REFER TO INTERIOR ELEVATIONS FOR HALL BASE FINISH
5. THE FLOOR LANDING SHALL NOT BE MORE THAN 1/4" LOWER THAN THE THRESHOLD OF THE DOORWAY (803E2.4.1)

HEALTH DEPT. NOTES

1. FLOOR SURFACES IN ALL AREAS IN WHICH FOOD IS PREPARED, PACKAGED, OR STORED, WHERE ANY UTENSILS IS WASHED, WARE RETURN OR GARBAGE IS STORED, WHERE UNIVERSAL FACILITIES ARE LOCATED, IN ALL TOILET AND HAND WASHING AREAS AND IN EMPLOYEE CHANGE AND STORAGE AREAS SHALL BE OF AN APPROVED TYPE THAT CONTAINS UP THE WALL FIVE (5) INCHES IN A SEAMLESS MANNER, FORMING A 1/8" RICK RADIUS CORNER AS AN INTERNAL JUT. SOME SLIM FOOT CORNER, QUARRY AND METAL FORMER COVER BASES ARE GENERALLY ACCEPTABLE. SAMPLES OF THE PROPOSED COVER BASE MUST BE SUBMITTED AND APPROVED PRIOR TO INSTALLATION.
2. CONCRETE AND TILE SEALER NEEDS TO BE PENETRATING, CLEAR, BREAKE AND ACID RESISTANT. IT IS IMPORTANT THAT ALL SEALERS ARE ALLOWED TO CURE BEFORE FOOD IS STORED OR PREPARED, TO PREVENT CONTAMINATION OF FOOD BY THE CHIPPED VAPORS. SEALER TO BE CONFORM TO A.C.I. 10.113.
3. APPROVED FINISHES MUST BE PROVIDED TO A DISTANCE OF 3" OUT FROM SERVICE AREAS.
4. NON-SKID AGENTS ARE RESTRICTED TO HIGH TRAFFIC AREAS ONLY. FLOORING SHALL BE SMOOTH UNDER EQUIPMENT.
5. WATERPROOF MATERIAL TO BE PROVIDED TO A HEIGHT OF 4" BEHIND HANDSINKS, AND AT LEAST 6" BEHIND ALL OTHER SINKS.



WESTER STATES ENGINEERING, INC.
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SANTA ANA, CALIF. 92701
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WWW.WESTERSTATES.COM

CONSULTANT'S SEAL

REVISIONS

NO.	DESCRIPTION	DATE
1		
2		
3		
4		

OWNER NAME & ADDRESS

THE GREEN GROUP
341 ALESSANDRO BLVD
RIVERSIDE, CA

PROJECT NAME & ADDRESS

INTERIOR REMODEL TO EXISTING C-STORE
341 ALESSANDRO BLVD
RIVERSIDE, CA

DATE TO CONTRACTOR

DATE TO OWNER

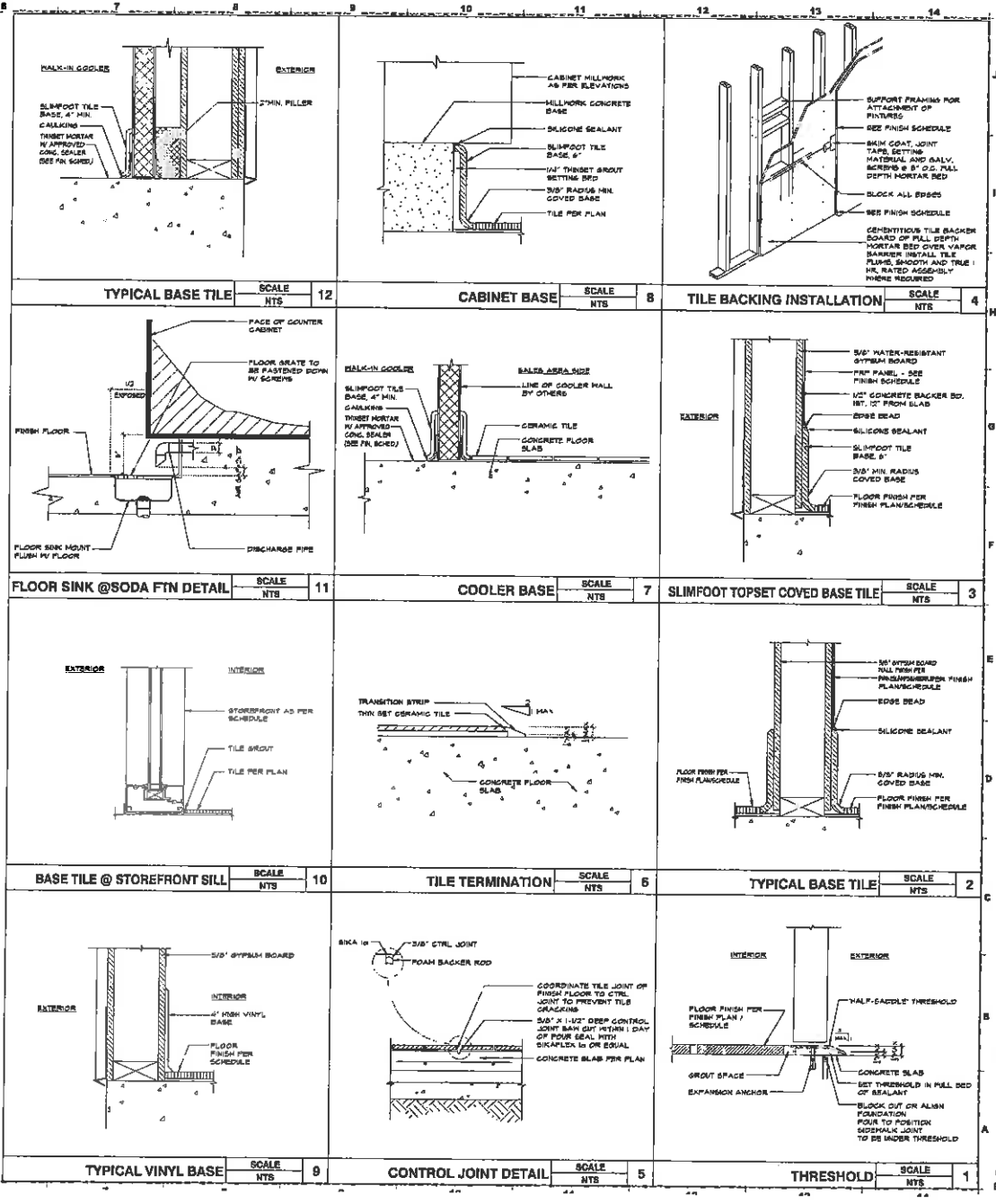
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DESIGNED BY: WSE
DRAWN BY: SA
CHECKED BY: JK
DATE DRAWN: 03.11.15
SCALE: 1/4"=1'-0"
SHEET TITLE: FLOOR FINISH PLAN

SHEET NUMBER

A-7.0
JOB No: E91415
CUP No: CUP NO.



Western States
Engineering, Inc.
A PROFESSIONAL CORPORATION
4845 E. LA PALMA AVE., 707
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NO.	DESCRIPTION	BY	DATE
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REVISIONS

NO.	DESCRIPTION	BY	DATE
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OWNER NAME & ADDRESS
THE GREEN GROUP
341 ALBERMARLE BLVD
BERKELEY, CA

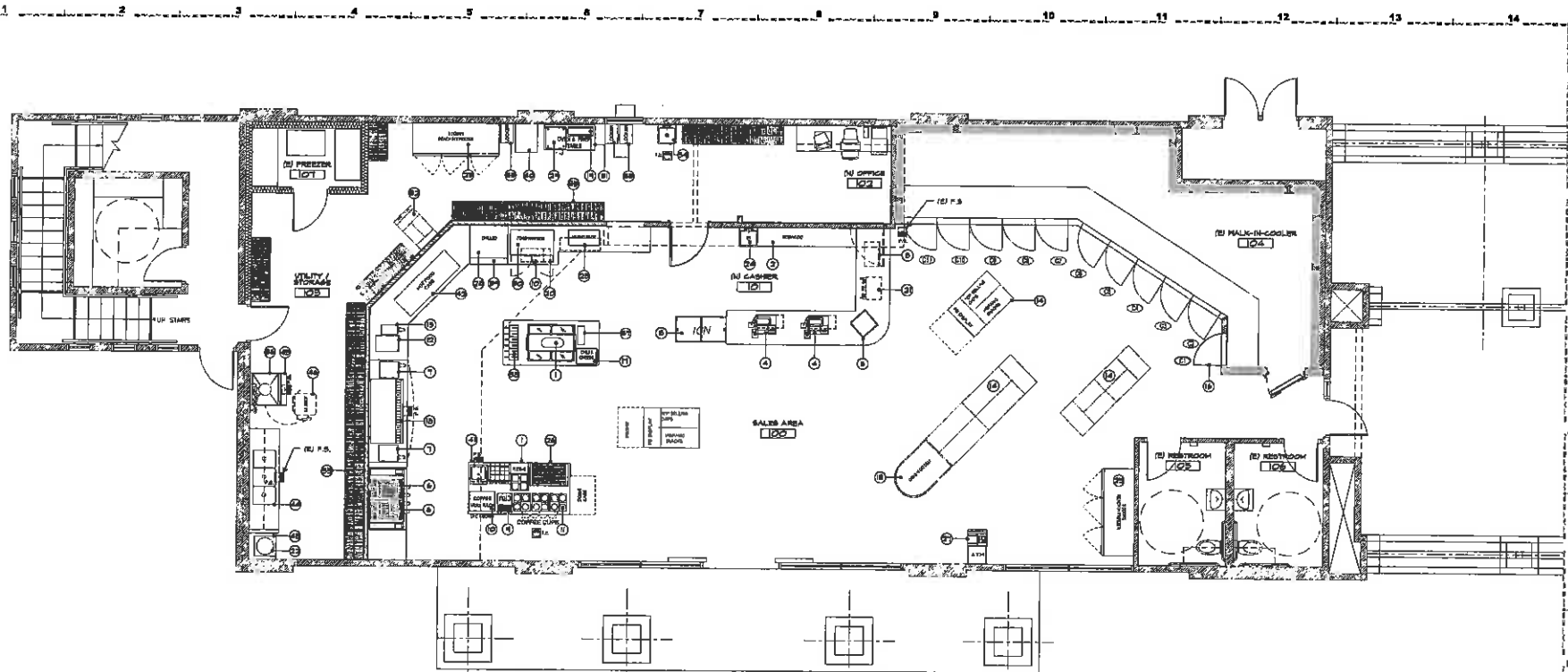
PROJECT NAME & ADDRESS
INTERIOR REMODEL
TO EXISTING C-STORE
311 ALBERMARLE BLVD
BERKELEY, CA

NOTE TO CONTRACTOR
CONCRETE IS PERMITTED TO BE PLACED IN 1 DAY OF TROWELING.

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DRAWN BY: WS
CHECKED BY: WS
DATE DRAWN: 06-25-14
SCALE: NTS
SHEET TITLE: FINISH DETAILS

SHEET NUMBER
A-7.1
JOB NO. E91415
CUP NO.



1 EQUIPMENT PLAN & SCHEDULE
 SCALE: 1/4" = 1'-0"

EQUIPMENT SCHEDULE												EQUIPMENT SCHEDULE																			
NO.	QTY	CTK	SC	FW	CTK	SC	FW	ITEM	MANUFACTURER	MODEL NO.	VOLTAGE	PH	AMPERAGE	PCN	REMARKS	NO.	QTY	CTK	SC	FW	CTK	SC	FW	ITEM	MANUFACTURER	MODEL NO.	VOLTAGE	PH	AMPERAGE	PCN	REMARKS
1	1	X						STAFF CONDUIT BAR	HOUSING/STATION		120V	1	0.5 AMP		4" X 4" TO 8" X 8"	1	1	X						FRID REFR	BLANCO	FRID-14	120V	1	1.5 AMP		APACKET ASSEMBLY, DRAM DIRECT
2	1	X						10'-0" TOROCCO BACK BAR	HYDRA-BOND	SET 100-100	120V	1	1.5 AMP		10'-0" TOROCCO BACK BAR	2	2	X						3 DOORS REFRIG-FRIZER	WASSERSTEIN	3 1000D	120V	1	1.5 AMP		UNIT IV GRID 4 1/2" X 1/2" FLG
3	1	X						ICE CREAM NOVELTY	ADVANCED-ICEBOX		120V	1	0.2 AMP		ICE CREAM NOVELTY	2	1	X						WATER ROLLER SHAL	STAR	WATER-100	120V	1	1.5 AMP		WATER ROLLER TO HAVE A SINGLE SHALE WATER FROM-8"
4	2		V					P.O.D. SYSTEM	RADWAY STANDARD		120V	1	1.1 AMP		P.O.D. SYSTEM	2	2		V					VEHICOR SUPPLY	VEHICOR	VEHICOR	120V	1	1.1 AMP		REQUIRED DATA LINE 4 DECATED ELECTRICAL SUPPLY
5	1		V					PA-COVER LOTTERY	TAKE-A-TAKE	MAQUA ULTIMATE	120V	1	1.1 AMP		PA-COVER LOTTERY	1	1		V					FREE STANDING DELI CASE	WATER-DEL	WATER-DEL	120V/200V	1	1.50		3400 PPK, 2550 CFM, DOOR-ACTIVATED
6	1		V					FRIDGE REFRIG DISPENSER	ICEE	FRID-01 FLAVOR	200-250V	1	14 AMP		FRIDGE REFRIG DISPENSER	1	1		V					DELUXE CONVECTION OVEN	MOORE-DEL	MOORE-194-4	200V/250V	1	20 AMP	14"	CONVECTION OVEN, 250V ONLY, BY ORDER
7	1		V					TOMMYN CUP 1 LID DISPENSER	AMERICAN	AM-100-1000-1	120V	1	1.5 AMP		TOMMYN CUP 1 LID DISPENSER	1	1		V					FOOD WARMER	WALTO	WALTO-41	120V/240V	1	1.8 AMP		UNIT IV GRID 4 1/2" X 1/2" FLG
8	1	X						SINGLE PACK CIG DISP	AMERICAN DISPLAY	AM-100-1000-1	120V	1	1.5 AMP		SINGLE PACK CIG DISP	1	1		X					PREPARATOR TABLE	WASSERSTEIN	WASSERSTEIN	120V/240V	1	1.5 AMP		30" X 48" X 36"
9	1	X						CREAM DISPENSER	FRANZONI	FRANZONI	120V	1	3 AMP		CREAM DISPENSER	1	1		X					WIRELESS FOOD WARMER	RESRAD	RESRAD	240V	1	5.5 AMP	YES	10" X 14" X 14"
10	1	X						COFFEE DISPENSER	FRANZONI	FRANZONI	120V	1	3 AMP		COFFEE DISPENSER	1	1		X					CONVECTION COOKER SERIAL	WASSERSTEIN	WASSERSTEIN	120V/240V	1	1.5 AMP		10" X 14" X 14"
11	1	X						COFFEE DISPENSER	FRANZONI	FRANZONI	120V	1	3 AMP		COFFEE DISPENSER	1	1		X					SET ONE COFF. DISP. (H&J)	ELANCO	ELANCO-01	120V	1	1.5 AMP		REFACKET ASSEMBLY, DRAM DIRECT
12	1							WATER LATTE M. HEAD	BLAN	BLAN-4	120V	1	4.5 AMP		WATER LATTE M. HEAD	1	1		X					WIRE SHELVING	WALTO	WALTO	120V	1	1.5 AMP		UNIT IV GRID 4 1/2" X 1/2" FLG
13	1							GRAB-N-GO DELI	DRYFO		120V	1	1.5 AMP		GRAB-N-GO DELI	1	1		X					BURR ICE MAKER-1000S	ICE-O-MATIC	ICE-O-MATIC	200-250V	1	1.5 AMP	YES	25" PPK WATER LINE 4" W/IN BUILT FOOT AT WATER SUPPLY, SERVICE DRAIN TO FLOOR GRN
14	1							CONDENSER	DRYFO		120V	1	1.5 AMP		CONDENSER	1	1		X					WTRAL W/KEY			120V	1	1.5 AMP		
15	1							ICEB CONVEYOR	PLANTICA 1 PROTECTO	PLANTICA-10	120V	1	1.5 AMP		ICEB CONVEYOR	1	1		X					CONVEYOR COMPRESSOR	SEVIER EXPRESS	SEVIER	120V	1	1.5 AMP		
16	1	X						COOLER DOOR	HATCO		120V	1	1.5 AMP		COOLER DOOR	1	1		X					3 SET CASE WARMER	WASSERSTEIN	WASSERSTEIN	120V	1	1.5 AMP		
17	1							CHILL & CUPPER DISPENSER	FOR DEL. REF TOP	FLAVOR-FRESH	120V	1	4.0 AMP		CHILL & CUPPER DISPENSER	1	1		X					WATER ROLLER SHAL	STAR	WATER-100	120V	1	1.5 AMP		UNIT IV GRID 4 1/2" X 1/2" FLG
18	1							35-VALVE KEY BEVERAGE DISPENSER	BRINCO	BRINCO	120V	1	4.0 AMP		35-VALVE KEY BEVERAGE DISPENSER	1	1		X					2ND 3" PVC DRAIN FITTING FROM DRAIN PAN			120V	1	4.0 AMP		
19	1							SUMMIT BAR TENDER	FRANZONI	FRANZONI	120V	1	3 AMPS		SUMMIT BAR TENDER	1	1		X					TOP-LINK BY VENDOR			120V	1	3 AMPS		
20	1							70-80 WTR HOLDEN (BELOW)	PLANTICA 1 PROTECTO	PLANTICA-10	120V	1	1.5 AMP		70-80 WTR HOLDEN (BELOW)	1	1		X					DRAM SWAPPER CASE	WASSERSTEIN	WASSERSTEIN	120V	1	1.5 AMP		UNIT IV GRID 4 1/2" X 1/2" FLG
21	1	X						CASH CONTROL SAFE	MC	MC	120V	1	4 AMP		CASH CONTROL SAFE	1	1		X					NOT FOOD CASE (S&G 1/2")	AMERICAN FOOD SERVICE	AFS-1	120V	1	4.0 AMP		
22	1							WATER HEATER	PAULONA	PAULONA	120V	1	4 AMP		WATER HEATER	1	1		X					BU HOT DRN	GENERAL COMPRESSOR	GEN-100	120V	1	4.0 AMP		10" X 17" X 17" W/IN 5"
23	1							WATER HEATER	PAULONA	PAULONA	120V	1	4 AMP		WATER HEATER	1	1		X					REF 2-COMPARTMENT SK	GENERAL COMPRESSOR	GEN-100	120V	1	4.0 AMP		10" X 17" X 17" W/IN 5"
24	1							WATER HEATER	PAULONA	PAULONA	120V	1	4 AMP		WATER HEATER	1	1		X					DRAM SWAPPER CASE	WASSERSTEIN	WASSERSTEIN	120V	1	4.0 AMP		10" X 17" X 17" W/IN 5"
25	1							WATER HEATER	PAULONA	PAULONA	120V	1	4 AMP		WATER HEATER	1	1		X					DRAM SWAPPER CASE	WASSERSTEIN	WASSERSTEIN	120V	1	4.0 AMP		10" X 17" X 17" W/IN 5"

REVISIONS
 NO. DESCRIPTION BY DATE
 1
 2
 3
 4

REVISIONS
 NO. DESCRIPTION BY DATE
 1
 2
 3
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OWNER NAME & ADDRESS
 THE GREEN GROUP

PROJECT NAME & ADDRESS
 INTERIOR REMODEL TO EXISTING C-STORE
 311 ALLEGRAHO BLVD
 REVERSHO, CA

DESIGNED BY: JAS
DRAWN BY: JAS
CHECKED BY: JAS
DATE DRAWN: 03/26/15
SCALE: 1/4" = 1'-0"

EQUIPMENT PLAN & SCHEDULE

JOB NO: E91415
CUP NO.: A-9.0