

# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

## STAFF REPORT

**AGENDA ITEM:** 3.1

**HEARING DATE:** May 12, 2022

**CASE NUMBER:** ZAP1086BD22 – AT&T (Representative: Smartlink)

**APPROVING JURISDICTION:** City of Palm Desert

**JURISDICTION CASE NO:** CUP22-0001 (Conditional Use Permit)

**LAND USE PLAN:** 2004 Bermuda Dunes Airport Land Use Compatibility Plan

Airport Influence Area: Bermuda Dunes Airport

Land Use Policy: Compatibility Zone C

Noise Levels: 55 - 60 CNEL contour

**MAJOR ISSUES:** None

**RECOMMENDATION:** Staff recommends that the Conditional Use Permit be found **CONDITIONALLY CONSISTENT**, subject to the conditions included herein, and such additional conditions as may be required by the Federal Aviation Administration Obstruction Evaluation Service.

**PROJECT DESCRIPTION:** A proposal to establish a 65-foot-tall mono-palm tree wireless communications facility with a 562 square foot equipment shelter on 0.81 acres.

**PROJECT LOCATION:** The site is located at 39750 Garand Lane, approximately 8,020 feet westerly of Runway 10-28 at Bermuda Dunes Airport.

### **BACKGROUND:**

Non-Residential Intensity: Pursuant to the 2004 Bermuda Dunes Airport Land Use Compatibility Plan, the project site is located within Compatibility Zone C which restricts average intensity to 75 people per acre, and a maximum single acre intensity of 150 people. The proposed mono-palm tree wireless facility will not generate any occupancy.

Prohibited and Discouraged Uses: The applicant does not propose any uses specifically prohibited or discouraged in Compatibility Zone C of the Bermuda Dunes Airport Influence Area.

Noise: The site is located within the 55-60 CNEL contour range from aircraft noise. The proposed mono-palm tree wireless facility will not generate any occupancy. Therefore, no special measures are required to mitigate aircraft-generated noise.

Part 77: The elevation of Runway 10-28 at its westerly terminus is approximately 73 feet above mean sea level (AMSL). At a distance of approximately 8,020 feet from the runway, FAA review would be required for any structures with top of roof exceeding 153 feet AMSL. The project's site elevation is 112 feet AMSL, and the maximum height of the existing building is 65 feet, for a maximum top point elevation of 177 feet AMSL. Therefore, review of the structure for height/elevation reasons by the FAA Obstruction Evaluation Service (FAAOES) was required. The applicant has submitted Form 7460-1, and FAA OES has assigned Aeronautical Study No. 2022-AWP-1533-OE to this project and is currently in a "work in progress" status.

Open Area: The site is located within Compatibility Zone C of the Bermuda Dunes Airport Influence Area, which requires projects 10 acres or larger to designate 20% of project area as ALUC qualifying open area that could potentially serve as emergency landing areas. Since the overall project size is 0.81 acres, the open area requirement is not applicable to this project.

**CONDITIONS:**

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
  - (a) Any use or activity which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use or activity which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use or activity which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, outdoor production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Children's schools, day care centers, libraries, hospitals, nursing homes, and buildings with more than three aboveground habitable floors.
  - (f) Highly noise-sensitive outdoor nonresidential uses.

- (g) Any use which results in a hazard to flight, including physical (e.g., tall objects), visual, and electronic forms of interference with the safety of aircraft operations.
- 3. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property and be recorded as a deed notice.
- 4. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the stormwater basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at [RCALUC.ORG](http://RCALUC.ORG) which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

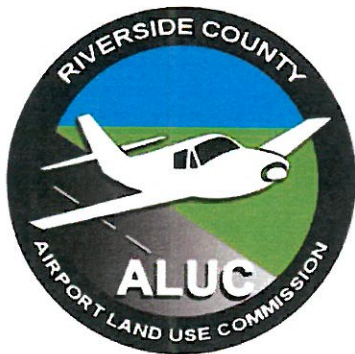
# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

# NOTICE

**THERE IS AN AIRPORT NEARBY.  
THIS STORM WATER BASIN IS DESIGNED TO HOLD  
STORM WATER FOR ONLY 48 HOURS AND  
NOT TO ATTRACT BIRDS**

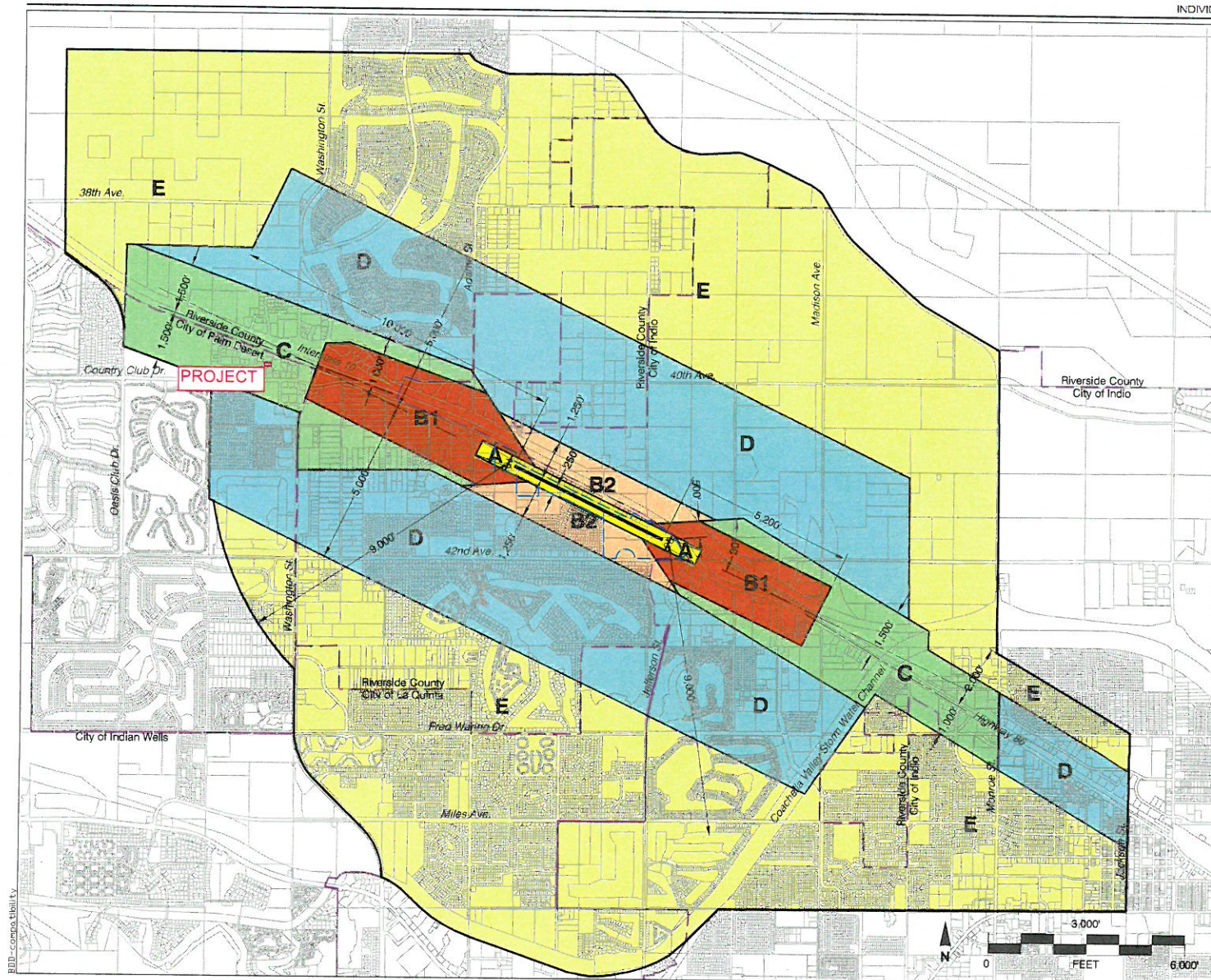
**PROPER MAINTENANCE IS NECESSARY TO AVOID  
BIRD STRIKES**



**IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:**

**Name:** \_\_\_\_\_

**Phone:** \_\_\_\_\_



**Legend**

- Compatibility Zones**
- Airport Influence Area Boundary
  - Zone A
  - Zone B1
  - Zone B2
  - Zone C
  - Zone D
  - Zone E
- Boundary Lines**
- Airport Property Line
  - City Limits

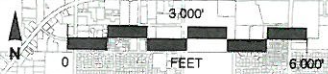
**Note**  
 Southwestern edge of Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

See Chapter 2, Table 2A for compatibility criteria associated with this map.

**Riverside County**  
**Airport Land Use Commission**  
**Riverside County**  
**Airport Land Use Compatibility Plan**  
**Policy Document**  
 (Adopted December 2004)

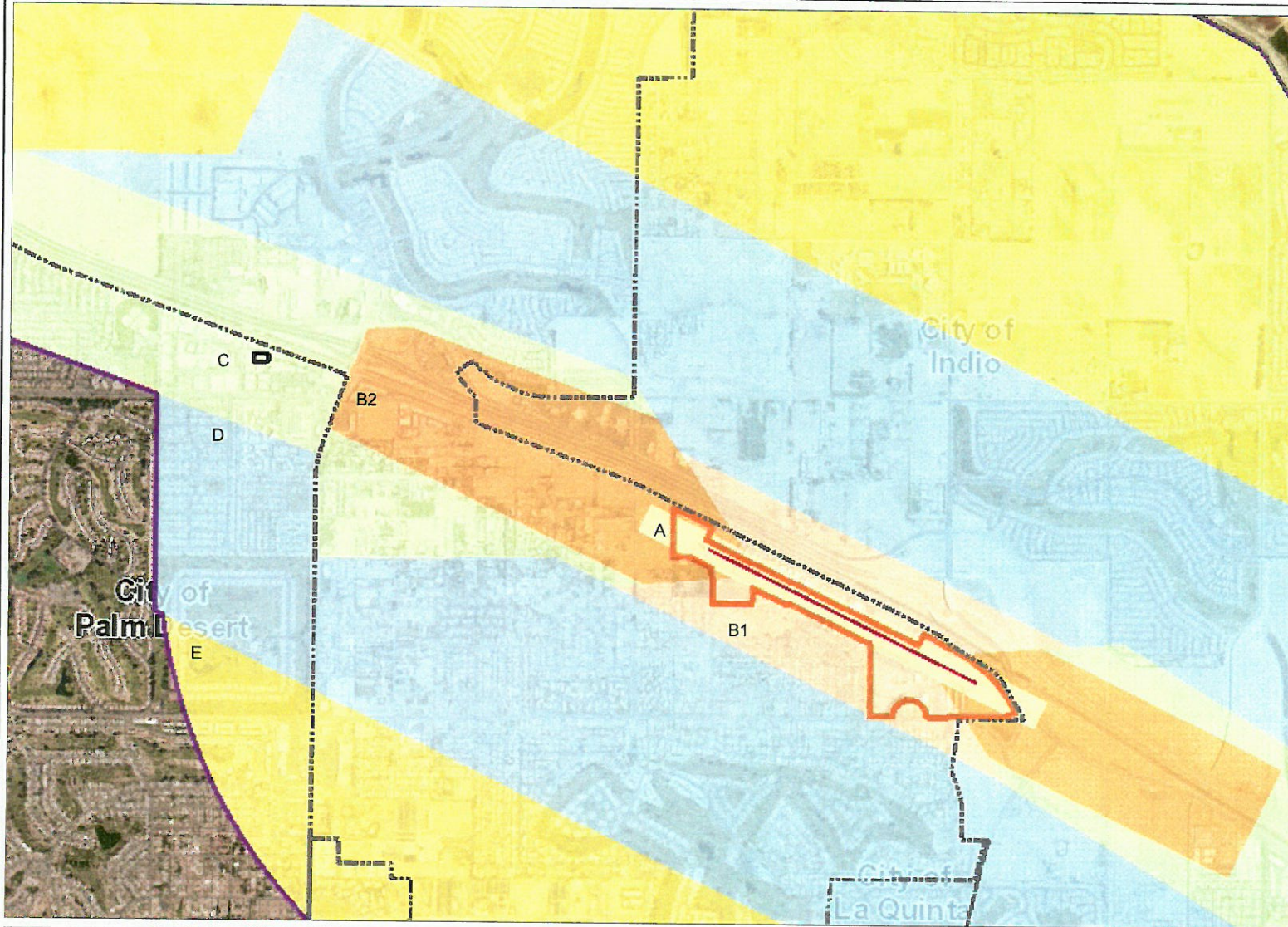
Map BD-1

**Compatibility Map**  
 Bermuda Dunes Airport



BDD - compatibility

# Map My County Map



- Legend**
- Runways
  - Airports
  - Airport Influence Areas
  - Airport Compatibility Zones**
  - OTHER COMPATIBILITY ZONE
  - A
  - A-EXC1
  - B1
  - B1-APZ I
  - B1-APZ I-EXC1
  - B1-APZ II
  - B1-APZ II-EXC1
  - B1-EXC1
  - B2
  - B2-EXC1
  - C
  - C1
  - C1-EXC1
  - C1-EXC3
  - C1-EXC4
  - C1-HIGHT
  - C2
  - C2-EXC1
  - C2-EXC2
  - C2-EXC3
  - C2-EXC5
  - C2-EXC6



**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

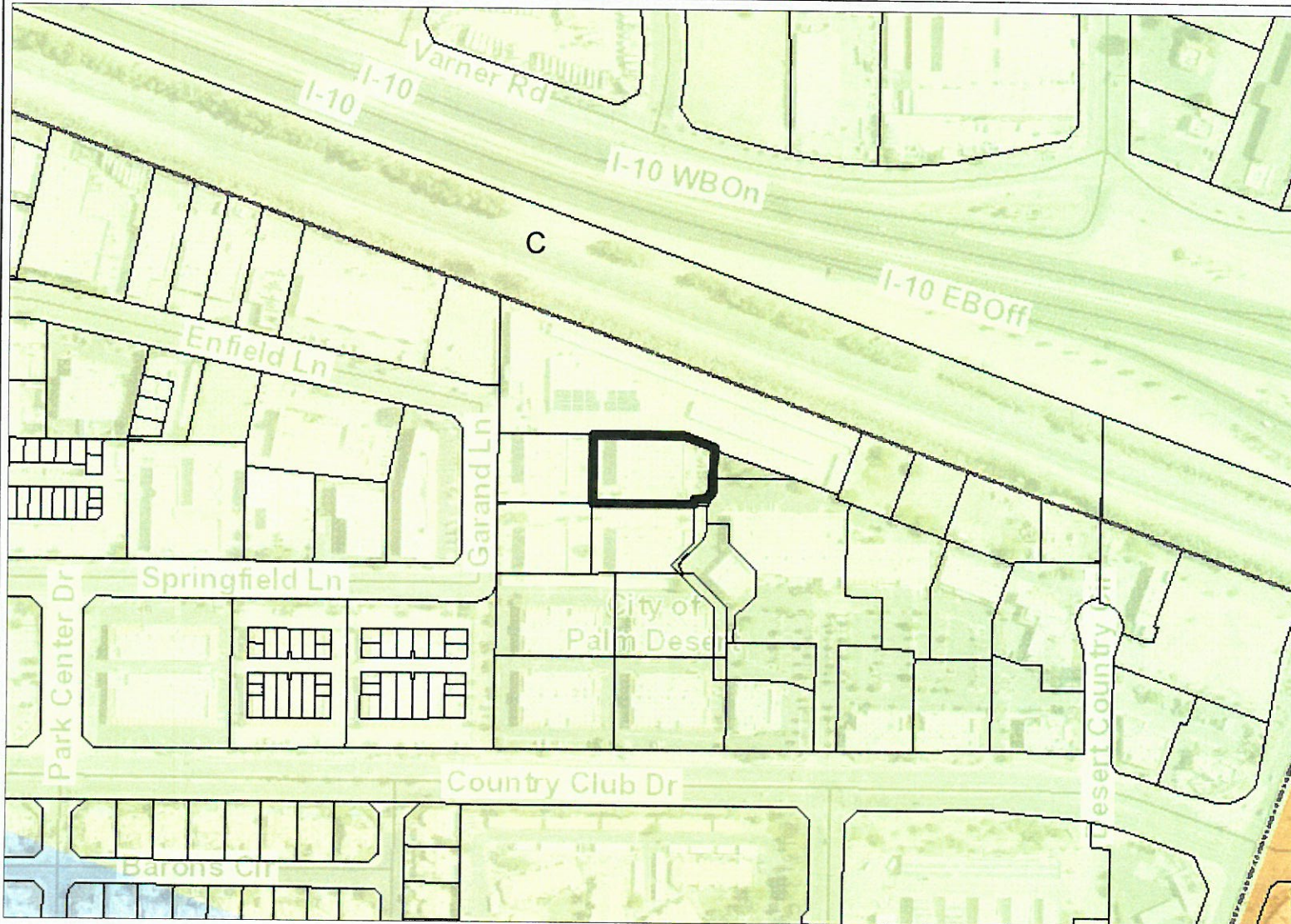
**Notes**



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# Map My County Map



**Legend**

- Parcels
- Runways
- Airports
- Airport Influence Areas

**Airport Compatibility Zones**

- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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**Notes**

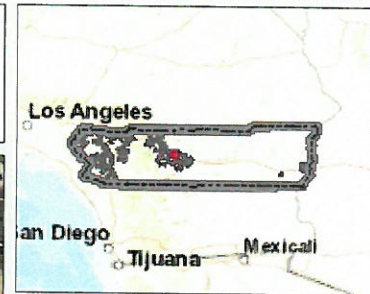
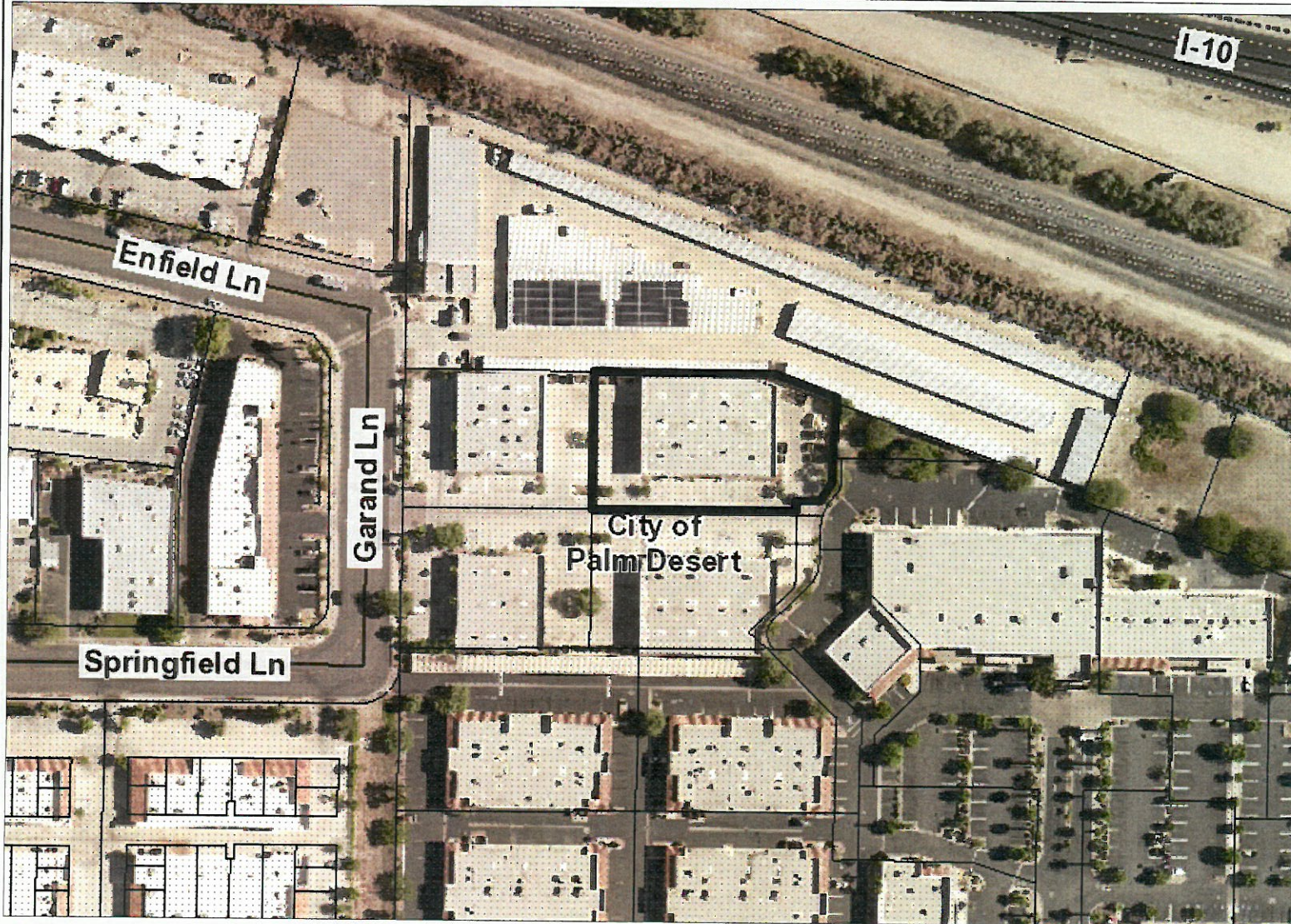


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# Map My County Map



## Legend

-  Parcels
-  County Centerline Names
-  County Centerlines
-  Blueline Streams
-  City Areas
-  World Street Map

## Notes



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0 192 385 Feet

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# Map My County Map



- Legend**
- Blueline Streams
  - ▨ City Areas
  - World Street Map

**City of  
Indio**

**City of  
Palm Desert**

**City of  
Indian Wells**



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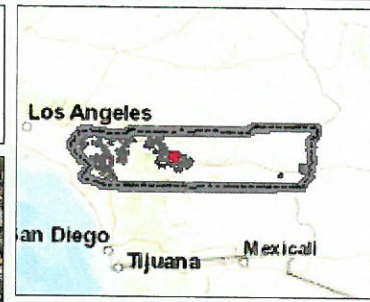
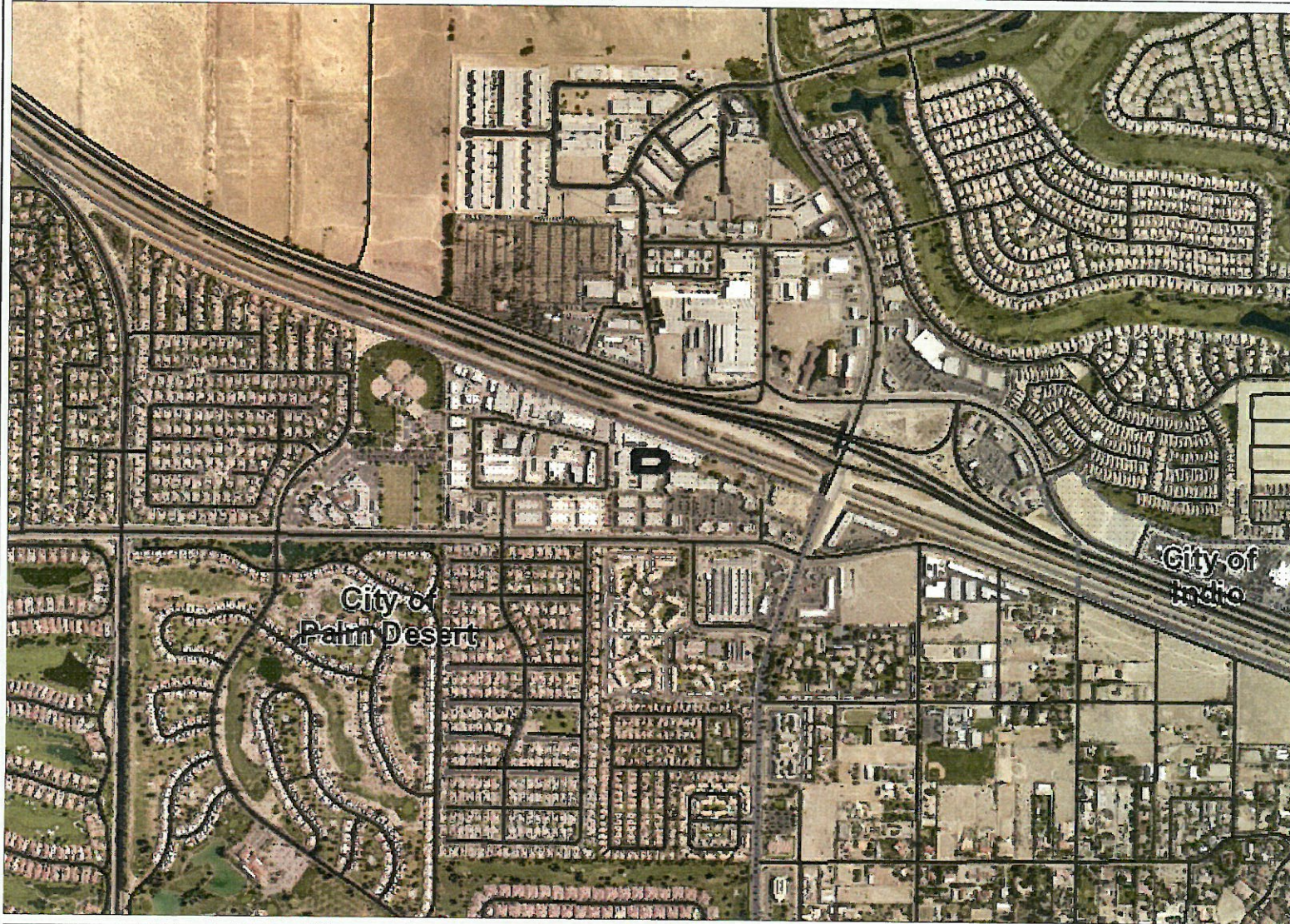
**Notes**



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# Map My County Map



- Legend**
- County Centerline Names
  - County Centerlines
  - Blueline Streams
  - City Areas
  - World Street Map



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**Notes**



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# Map My County Map



- Legend**
- Parcels
  - County Centerline Names
  - County Centerlines
  - Blueline Streams
  - City Areas
  - World Street Map



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**Notes**



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**CODE COMPLIANCE**

- ALL WORKS AND MATERIALS SHALL BE PERFORMED AND INSTALLED IN ACCORDANCE WITH THE CURRENT EDITIONS OF THE FOLLOWING CODES AS ADOPTED BY THE LOCAL GOVERNING AUTHORITIES NOTHING IN THESE PLANS IS TO BE CONSIDERED TO PERMIT WORK NOT CONFORMING TO THE LATEST EDITIONS OF THE FOLLOWING CODES:
- 1. 2019 CALIFORNIA BUILDING CODE
  - 2. 2019 CALIFORNIA ELECTRICAL CODE
  - 3. 2019 CALIFORNIA FIRE CODE
  - 4. 2019 CALIFORNIA MECHANICAL CODE
  - 5. 2019 CALIFORNIA PLUMBING CODE
  - 6. 2019 CALIFORNIA ENERGY CODE
  - 7. COUNTY OF COASTAL / OCEAN LAND USE ORDINANCE - TITLE 23
  - 8. COUNTY OF FIRE CODE ORDINANCE - TITLE 19
  - 9. COUNTY OF LAND USE ORDINANCE - TITLE 22
  - 10. COUNTY OF BUILDING AND CONSTRUCTION ORDINANCE - TITLE 19

**PROJECT TEAM**

**CLIENT REPRESENTATIVE**

COMPANY: SMARTLINK LLC  
 ADDRESS: 3300 IRVINE AVENUE, SUITE 300  
 NEWPORT BEACH, CA 92660  
 CONTACT: ALEXIS DUNLAP  
 PHONE: (949) 838-7213  
 EMAIL: alexis.dunlap@smartlinkgroup.com

**CONSTRUCTION MANAGER**

COMPANY: AT&T  
 ADDRESS: 1452 EDINGER AVE  
 TUSTIN, CA 92780  
 CONTACT: CHRISTIE M. ASARI-PRICE  
 PHONE: (714) 287-2628  
 EMAIL: CH0897@att.com

**SITE ACQUISITION**

COMPANY: SMARTLINK LLC  
 ADDRESS: 3300 IRVINE AVENUE, SUITE 300  
 NEWPORT BEACH, CA 92660  
 CONTACT: ALEXIS DUNLAP  
 PHONE: (949) 838-7213  
 EMAIL: alexis.dunlap@smartlinkgroup.com

**ATT PROJECT MANAGER**

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 TUSTIN, CA 92780  
 CONTACT: CHRISTIE M. ASARI-PRICE  
 PHONE: (714) 287-2628  
 EMAIL: CH0897@att.com

**ZONING**

COMPANY: SMARTLINK LLC  
 ADDRESS: 3300 IRVINE AVENUE, SUITE 300  
 NEWPORT BEACH, CA 92660  
 CONTACT: ALEXA STRASHEM  
 PHONE: (949) 488-0069  
 EMAIL: alexa.straschem@smartlinkgroup.com

**APPLICANT**

COMPANY: AT&T  
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 TUSTIN, CA 92780  
 CONTACT: CHRISTIE M. ASARI-PRICE  
 PHONE: (714) 287-2628  
 EMAIL: CH0897@att.com

**ENGINEER**

COMPANY: CASA INDUSTRIES, INC.  
 ADDRESS: 4430 E. MIRALOMA AVE, SUITE D  
 ANAHEIM, CA 92807  
 CONTACT: JULIUS SANTIAGO  
 PHONE: (714) 553-8889  
 EMAIL: JSANTIAGO@CASAINC.COM

**RF ENGINEER**

COMPANY: AT&T  
 ADDRESS: 7390 E. SANTA CLARA ST.,  
 ROOM 217  
 VENTURA, CA 93001  
 CONTACT: SANDHEE MANGAT  
 PHONE: (805) 312-1684  
 EMAIL: SMANGAT@att.com

**SITE INFORMATION**

**APPLICANT / LESSEE**



**PROPERTY OWNER**

OWNER: HERNADEZ FAMILY TRUST  
 ADDRESS: 39750 GARAND LANE  
 PALM DESERT, CA 92211  
 CONTACT: MARIO HERNANDEZ  
 PHONE: 760-535-4976

LATITUDE: 33° 45' 36.74" N (33.7599289)  
 LONGITUDE: 118° 18' 23.24" W (-118.3064967)

LATAING TYPE: NAD 83  
 GROUND ELEVATION: 112.5 AMSL  
 ABOVE GROUND LEVEL: 65' A.G.L.  
 APN #: 626-41406  
 AREA OF CONSTRUCTION: 4545.25 SQ. FT.  
 ZONING / JURISDICTION: CITY OF PALM DESERT  
 SERVICE INDUSTRIAL WITH FREEMAN / COMMERCIAL OVERLAY (DDE)  
 CURRENT ZONING: UNMANNED TELECOMMUNICATIONS FACILITY  
 PROPOSED USE: FACILITY IS UNMANNED AND NOT FOR HUMAN HABITATION.  
 HANDICAP REQUIREMENTS: HANDICAPPED ACCESS NOT REQUIRED.  
 GROUND LEASE AREA: 562 SQ. FT.  
 AIRPL. LEASE AREA: 180 SQ. FT.



**AT&T**

Your world. Delivered

SITE NUMBER: CSL02434 - NSB

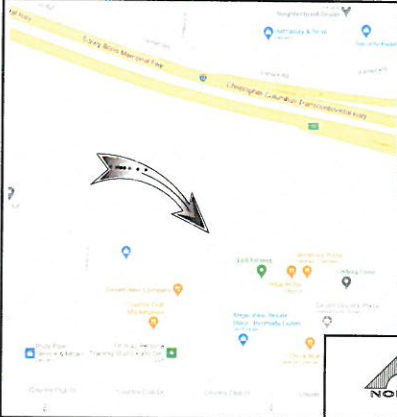
SITE NAME: GARAND

FA#: 13024049

USID#: TBD

39750 GARAND LANE,  
 PALM DESERT, CA 92211  
 RIVERSIDE COUNTY

**VICINITY MAP**



**LOCAL MAP**



**DRIVING DIRECTIONS**

DIRECTIONS FROM AT&T OFFICE:  
 TURN RIGHT ONTO EDINGER AVE. USE THE LEFT 2 LANES TO TURN LEFT ONTO THE CA-261 N RAMP. KEEP RIGHT AT THE FORK. FOLLOW SIGNS FOR CA-261 N AND MERGE ONTO CA-261 N. MERGE ONTO CA-261 N. USE ANY LANE TO MERGE ONTO CA-241 N. USE THE RIGHT 2 LANES TO TAKE EXIT 33A TO MERGE ONTO CA-31 E TOWARD RIVERSIDE. KEEP LEFT TO STAY ON CA-31 E. USE THE RIGHT 2 LANES TO TAKE EXIT 65B TO MERGE ONTO CA-80 E TOWARD I-215 S/SAN DIEGO/INDO. CONTINUE ONTO CA-80 E / I-215 S. KEEP LEFT AT THE FORK TO CONTINUE ON CA-80 E. KEEP LEFT TO STAY ON CA-80 E. USE ANY LANE TO MERGE ONTO I-10 E. TAKE EXIT 137 FOR WASHINGTON ST. USE THE RIGHT LANE TO TURN RIGHT ONTO WASHINGTON ST. TURN RIGHT ONTO COUNTRY CLUB DR. TURN RIGHT ADD ON PARK CENTER DRIVE. TURN RIGHT ON SPRINGFIELD LANE. TURN LEFT ON GARAND LANE. SITE WILL BE ON THE RIGHT.

**LEGAL DESCRIPTION**

SEE SURVEY SHEETS FOR LEGAL DESCRIPTION

**APPROVALS**

THE FOLLOWING PARTIES HEREBY APPROVE AND ACCEPT THESE DOCUMENTS. I AUTHORIZE THE SUBCONTRACTOR TO PROCEED WITH THE CONSTRUCTION DESCRIBED HEREIN. ALL DOCUMENTS ARE SUBJECT TO REVIEW BY THE LOCAL BUILDING DEPARTMENT. I MAY IMPOSE CHANGES OR MODIFICATIONS.

DISCIPLINE	SIGNATURE	DATE
AT&T RF ENGINEER		
AT&T OPERATIONS		
SITE ACQUISITION		
CONSTRUCTION MANAGER		
PROPERTY OWNER		
LOADING VENDOR		
PROJECT MANAGER		

**GENERAL CONTRACTOR NOTES**

DO NOT SCALE DRAWINGS  
 SUBCONTRACTOR SHALL VERIFY ALL PLANS & EXISTING DIMENSIONS & CONDITIONS ON THE JOB SITE & SHALL IMMEDIATELY NOTIFY THE ENGINEER IN WRITING OF ANY DISCREPANCIES BEFORE PROCEEDING WITH THE WORK OR BE RESPONSIBLE FOR SAME.

**GENERAL NOTES**

THE FACILITY IS UNMANNED AND NOT FOR HUMAN HABITATION. A TECHNICIAN WILL VISIT THE SITE AS REQUIRED FOR ROUTINE MAINTENANCE. THE PROJECT WILL NOT RESULT IN ANY SIGNIFICANT DISTURBANCE OR EFFECT ON DRAINAGE, NO SANITARY SEWER SERVICE, PORTABLE WATER, OR TRASH DISPOSAL IS REQUIRED AND NO COMMERCIAL SIGNAGE IS PROPOSED.

**PROJECT DESCRIPTION**

**INDOOR EQUIPMENT / WUC ENCLOSURE**

- "X"IT PROPOSES TO CONSTRUCT, OPERATE AND MAINTAIN AN UNMANNED WIRELESS COMMUNICATIONS FACILITY. THIS FACILITY WILL CONSIST OF THE FOLLOWING:
- INSTALL (P) 15' HIGH DELTA PALM MONOPOL
  - INSTALL (P) PANEL ANTENNAS (3 PER SECTOR)
  - INSTALL 36 (P) LITE RIBS AT ANTENNA LEVEL (12 PER SECTOR)
  - INSTALL 1 (P) 2" MM ANTENNA
  - INSTALL 4 (P) DC-8 SURGE SUPPRESSORS (SOUND)
  - INSTALL 1 (P) GPS ANTENNA
  - INSTALL 3 (P) DC-12 OUTDOOR
  - INSTALL (P) UTILITY PANELS
  - INSTALL 1 (P) DELTA WALK UP CABINET (WUC)
  - INSTALL 1 (P) 20W (37 GAL.) GENERATOR COMPACT DIESEL GENERATOR
  - INSTALL (P) SHROUS AND VINCS ON SOUTH AND EAST SIDE OF LEASE AREA

**DRAWING INDEX**

SHEET NO.	TITLE SHEET	SHEET TITLE
T-1	TITLE SHEET	
LS-1	SITE SURVEY	
LS-2	SURVEY NOTES	
A-1	SITE PLAN	
A-2	LEASE AREA PLAN/ANTENNA PLAN AND ANTENNA/RFU SCHEDULE	
A-3	ELEVATIONS	
A-4	ELEVATIONS	
E-1	PRELIMINARY LANDSCAPE PLAN	



THE INFORMATION CONTAINED IN THIS SET OF DRAWINGS IS PROPRIETARY & CONFIDENTIAL TO AT&T WIRELESS. ANY USE OR DISCLOSURE OTHER THAN AS IT RELATES TO AT&T WIRELESS IS STRICTLY PROHIBITED.



REV.	DATE	DESCRIPTION
2	01/1/22	100% REVISED WITH LANDSCAPE
1	11/1/21	100% REVISED ELEVATION PLAN
0	10/22/21	100% ZONING DRAWINGS
0	09/28/21	80% ZONING DRAWINGS
0	08/25/21	80% ZONING DRAWINGS

**NOT TO BE USED FOR CONSTRUCTION**

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT.

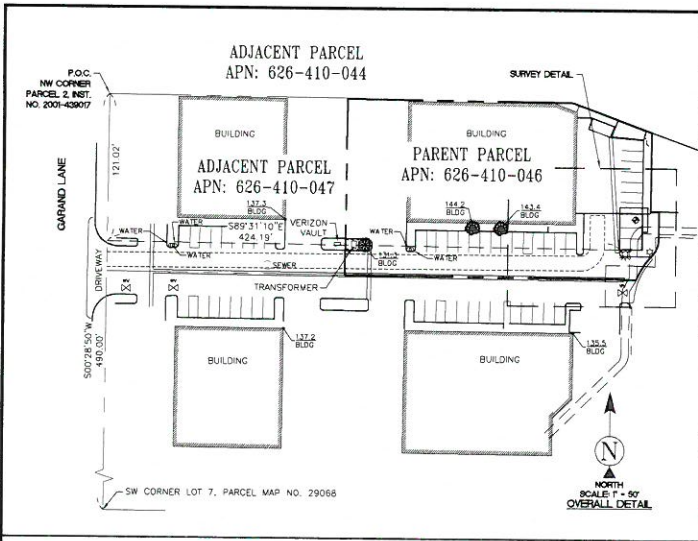
CSL02434  
 GARAND  
 39750 GARAND LANE,  
 PALM DESERT, CA 92211  
 MONOPALM (INDOOR)

DRAWN BY: EWS CHECKED BY: JS

SHEET TITLE: TITLE SHEET

SHEET NUMBER: T-1





**LEASE AREA LEGAL DESCRIPTION**

A PORTION OF PARCEL 3 AS SHOWN ON CERTIFICATE OF COMPLIANCE NO. 01-23 EVIDENCED BY DOCUMENT RECORDED SEPTEMBER 10, 2001 AS INSTRUMENT NO. 2001-439017 OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA, IN THE CITY OF PALM DESERT, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCING AT THE NORTHWEST CORNER OF PARCEL 2 OF SAID CERTIFICATE OF COMPLIANCE FROM WHICH THE SOUTHWEST CORNER OF LOT 7 OF PARCEL MAP NO. 29068, RECORDED IN PARCEL MAP BOOK 196, PAGES 8 THROUGH 9, OFFICIAL RECORDS OF SAID COUNTY BEARS SOUTH 00°28'50" WEST, 490.00 FEET; THENCE FROM SAID POINT OF COMMENCEMENT SOUTH 00°28'50" WEST ALONG THE WEST LINE OF SAID PARCEL 2, 121.02 FEET; THENCE DEPARTING SAID WEST LINE SOUTH 89°31'10" EAST, 424.19 FEET TO THE POINT OF BEGINNING.

THENCE NORTH 00°00'00" EAST, 30.00 FEET; THENCE NORTH 90°00'00" EAST, 18.75 FEET; THENCE SOUTH 00°00'00" EAST, 30.00 FEET; THENCE NORTH 90°00'00" WEST, 18.75 FEET TO THE POINT OF BEGINNING.

CONTAINING 56.3 SQUARE FEET (0.013 ACRES) OF LAND, MORE OR LESS.

**ACCESS NOTE**

RESERVING NONEXCLUSIVE RIGHT OF USE ACROSS LESSOR'S PROPERTY FOR NECESSARY APPURTENANCES TO CONSTRUCT, OPERATE, AND MAINTAIN A COMMUNICATION FACILITY FOR ITEMS SUCH AS, BUT NOT LIMITED TO, INGRESS, EGRESS, PARKING, VEHICULAR MANEUVERING, EQUIPMENT, AND UTILITIES.

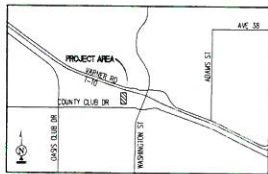
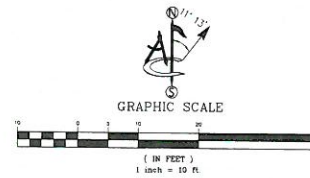
**AERIAL LEASE AREA LEGAL DESCRIPTION**

A PORTION OF PARCEL 3 AS SHOWN ON CERTIFICATE OF COMPLIANCE NO. 01-23 EVIDENCED BY DOCUMENT RECORDED SEPTEMBER 10, 2001 AS INSTRUMENT NO. 2001-439017 OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA, IN THE CITY OF PALM DESERT, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCING AT THE NORTHWEST CORNER OF PARCEL 2 OF SAID CERTIFICATE OF COMPLIANCE FROM WHICH THE SOUTHWEST CORNER OF LOT 7 OF PARCEL MAP NO. 29068, RECORDED IN PARCEL MAP BOOK 196, PAGES 8 THROUGH 9, OFFICIAL RECORDS OF SAID COUNTY BEARS SOUTH 00°28'50" WEST, 490.00 FEET; THENCE FROM SAID POINT OF COMMENCEMENT SOUTH 00°28'50" WEST ALONG THE WEST LINE OF SAID PARCEL 2, 121.02 FEET; THENCE DEPARTING SAID WEST LINE SOUTH 89°31'10" EAST, 424.19 FEET; THENCE NORTH 00°00'00" EAST, 30.00 FEET TO THE POINT OF BEGINNING.

THENCE NORTH 00°00'00" EAST, 5.00 FEET; THENCE NORTH 90°00'00" EAST, 23.00 FEET; THENCE SOUTH 00°00'00" EAST, 20.00 FEET; THENCE NORTH 90°00'00" WEST, 4.25 FEET; THENCE NORTH 00°00'00" WEST, 15.00 FEET; THENCE SOUTH 90°00'00" WEST, 18.75 FEET TO THE POINT OF BEGINNING.

CONTAINING 179 SQUARE FEET (0.004 ACRES) OF LAND, MORE OR LESS.



**SURVEYOR'S NOTES**

CONTOURS DERIVED FROM DIRECT FIELD OBSERVATIONS AND FOLLOW THE CURRENT NATIONAL MAP STANDARDS FOR VERTICAL ACCURACY.

THE BOUNDARY SHOWN HEREON IS PLOTTED FROM RECORD INFORMATION AND DOES NOT CONSTITUTE A BOUNDARY SURVEY OF THE PROPERTY.

ALL DISTANCES SHOWN HEREON ARE GRID DISTANCES.

SURVEYOR HAS NOT PERFORMED A SEARCH OF PUBLIC RECORDS TO DETERMINE ANY DEFECT IN TITLE ISSUED.

**LESSOR'S LEGAL DESCRIPTION**

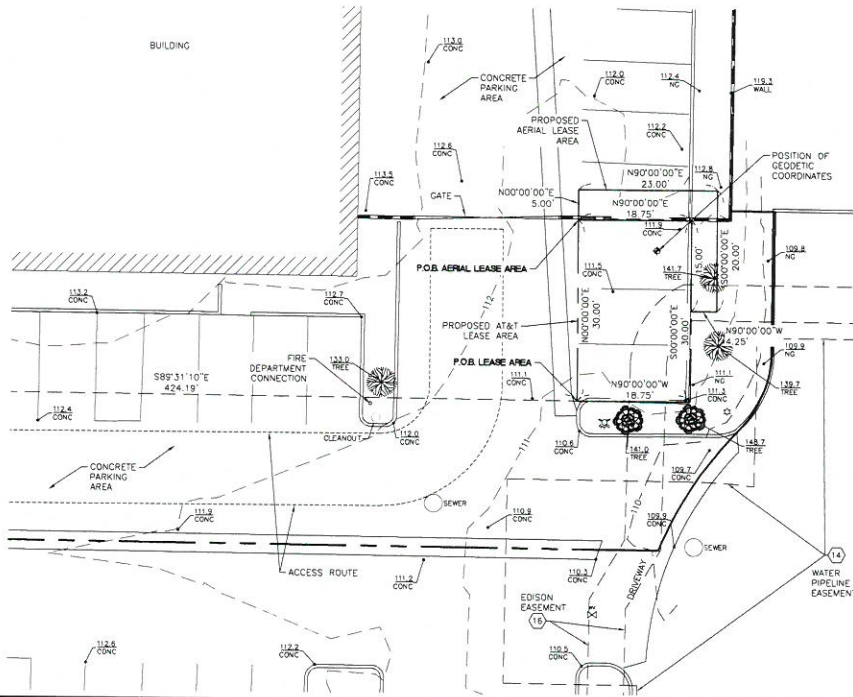
SEE SHEET LS-2

**NOTES CORRESPONDING TO RECORDS MATTERS**

SEE SHEET LS-2

POSITION OF GEODETIC COORDINATES  
 LATITUDE 33° 45' 35.74" (33.759928) NORTH (NAD83)  
 LONGITUDE 116° 18' 23.24" (116.306456) WEST (NAD83)  
 GROUND ELEVATION @ 111.6' (NAV088)

LEGEND	
BLDG	TOP OF BUILDING
CMU	CONCRETE MASONRY UNIT
CONC	CONCRETE
NC	NATURAL GRADE
WALL	TOP OF WALL
TREES	TREES
○	PALM TREES
○	UTILITY MANHOLES
○	UTILITY METER
⊗	FIRE HYDRANT
⊕	LIGHT POLE
○	POSITION OF GEODETIC COORDINATES
○	SPOT ELEVATION
○	WATER CONTROL VALVE
—	CMU WALLS
—	WROUGHT IRON FENCE
—	CURBLINE
—	SUBJECT PROPERTY LINE
—	ADJACENT PROPERTY LINE
—	LEASE AREA LIMITS
—	MAJOR CONTOUR INTERVAL
—	MINOR CONTOUR INTERVAL



**SURVEY DATE**

08/16/2021

**BASIS OF BEARING**

BEARINGS SHOWN HEREON ARE BASED UPON THE CALIFORNIA ZONE SIX STATE PLANE COORDINATE SYSTEM BASED ON THE NORTH AMERICAN DATUM OF 1983(2011) (EPOCH 2019.25) DETERMINED BY GLOBAL POSITIONING SYSTEM EQUIPMENT ON THE SMARTNET REFERENCE NETWORK.

**BENCHMARK**

PROJECT ELEVATIONS ESTABLISHED FROM GPS DERIVED ORTHOMETRIC HEIGHTS BY APPLICATION OF NGS 'GEOID 12B' MODELED SEPARATIONS TO ELLIPSOID HEIGHTS DETERMINED BY OBSERVATIONS OF THE 'SMARTNET' REAL TIME NETWORK. ALL ELEVATIONS SHOWN HEREON ARE REFERENCED TO NAVD88.

**GRID-TO-GROUND SCALE FACTOR NOTE**

ALL BEARINGS AND DISTANCES ARE BASED ON THE CALIFORNIA SIX STATE PLANE COORDINATE ZONE GRID. TO DERIVE GROUND DISTANCES DIVIDE BY 0.99998230

**FLOOD ZONE**

THIS PROJECT APPEARS TO BE LOCATED WITHIN FLOOD ZONE "X" ACCORDING TO FEDERAL EMERGENCY MANAGEMENT AGENCY FLOOD INSURANCE RATE MAP(S), MAP ID #06065C1620G, DATED 08/28/2008

**UTILITY NOTES**

SURVEYOR DOES NOT GUARANTEE THAT ALL UTILITIES ARE SHOWN OR THEIR LOCATIONS ARE DEFINITE. IT IS THE RESPONSIBILITY OF THE CONTRACTOR AND DEVELOPER TO CONTACT 811 AND ANY OTHER INVOLVED AGENCIES TO LOCATE ALL UTILITIES PRIOR TO CONSTRUCTION. REMOVAL, RELOCATION AND/OR REPLACEMENT IS THE RESPONSIBILITY OF THE CONTRACTOR.

1452 EDINGER AVENUE  
3RD FLOOR  
TUSTIN, CA 92780

ambit consulting  
428 MAIN STREET SUITE 206  
HUNTINGTON BEACH, CALIFORNIA 92648  
PH. (480) 659-4072

4435 E. HIRALOMA AVE. SUITE D  
ANHEIM, CALIFORNIA 92807

REV	DATE	DESCRIPTION	CHK
3	01/03/22	UPDATE	(PD)
2	11/22/21	DESIGN	(CK)
1	09/06/21	TITLE WORK	(PR)
0	08/20/21	INITIAL ISSUE	(CK)

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL SURVEYOR, TO ALTER THIS DOCUMENT.

CSLO2434  
39750 GARAND LANE  
PALM DESERT, CA 92211

SHEET TITLE  
SITE SURVEY

SHEET NUMBER  
LS-1

**TITLE REPORT LEGAL DESCRIPTION**

THE LAND REFERRED TO HEREIN BELONGS TO SITUATED PALM DESERT, IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS:  
ALL THAT CERTAIN REAL PROPERTY SITUATED IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, DESCRIBED AS FOLLOWS:  
PARCEL 3 AS SHOWN ON CERTIFICATE OF COMPLIANCE NO. 01-23 EVIDENCED BY DOCUMENT RECORDED SEPTEMBER 10, 2001 AS INSTRUMENT NO. 2001-430017 OF OFFICIAL RECORDS, BEING MORE PARTICULARLY DESCRIBED IN THE DOCUMENT AS FOLLOWS:  
PARCEL 3 AS SHOWN ON CERTIFICATE OF COMPLIANCE NO. 01-23 EVIDENCED BY DOCUMENT RECORDED SEPTEMBER 10, 2001 AS INSTRUMENT NO. 2001-430017 OF OFFICIAL RECORDS, BEING MORE PARTICULARLY DESCRIBED IN THE DOCUMENT AS FOLLOWS:

PARCEL B OF CITY OF PALM DESERT PARCEL MAP WAIVER 00-20 IN THE RIVERSIDE COUNTY, STATE OF CALIFORNIA AS SHOWN IN DOCUMENT RECORDED DECEMBER 18, 2000 AS INSTRUMENT NO. 2001-501788, OFFICIAL RECORDS OF SAID RIVERSIDE COUNTY, EXCEPTING THAT PORTION MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTH-WESTERLY CORNER OF SAID PARCEL B, THENCE ALONG THE NORTHERLY LINE OF SAID PARCEL B SOUTH 89°29'49" EAST, A DISTANCE OF 169.02 FEET;

THENCE ALONG THE NORTH-EASTERLY LINE OF SAID PARCEL B SOUTH 70°06'22" EAST, A DISTANCE OF 2.23 FEET TO A LINE PARALLEL WITH AND 0.74 FEET SOUTHERLY OF THE NORTHERLY LINE OF SAID PARCEL B, SAID LINE BEING HALF WAY NORTH AND SOUTH BETWEEN THE EXISTING BUILDINGS;

THENCE ALONG NORTH 89°29'49" WEST, A DISTANCE OF 171.12 FEET TO A POINT ON THE WESTERLY LINE OF SAID PARCEL B, THENCE ALONG THE WESTERLY LINE OF SAID PARCEL B NORTH 00°30'11" EAST, A DISTANCE OF 0.74 FEET TO THE POINT OF BEGINNING.

AND TOGETHER WITH A PORTION OF PARCEL 11 OF PARCEL MAP 29068-1 IN SAID CITY OF PALM DESERT, COUNTY OF RIVERSIDE, AS SHOWN IN PLAT MAP BOOK 195, PAGE 27-28, RECORDS OF SAID COUNTY MORE PARTICULARLY DESCRIBED AS FOLLOWS:  
BEGINNING AT THE NORTH-WESTERLY CORNER OF LOT A OF SAID PARCEL MAP 29068-1.

THENCE ALONG A NORTHERLY PROLONGATION OF THE WESTERLY LINE OF SAID LOT A,

THENCE ALONG NORTH 89°29'49" WEST, A DISTANCE OF 15.06 FEET TO A POINT ON THE SOUTH-WESTERLY LINE OF SAID PARCEL 11, THENCE ALONG THE SOUTH-WESTERLY LINE OF SAID PARCEL II SOUTH 70°06'22" EAST, A DISTANCE OF 85.73 FEET TO THE POINT OF BEGINNING.

THENCE ALONG SAID PARALLEL LINE NORTH 70°06'22" WEST, A DISTANCE OF 69.77 FEET.

THENCE NORTH 89°29'49" WEST, A DISTANCE OF 15.06 FEET TO A POINT ON THE SOUTH-WESTERLY LINE OF SAID PARCEL 11.

THENCE ALONG THE SOUTH-WESTERLY LINE OF SAID PARCEL II SOUTH 70°06'22" EAST, A DISTANCE OF 85.73 FEET TO THE POINT OF BEGINNING.

PARCEL B OF CITY OF PALM DESERT PARCEL MAP WAIVER 00-20 IN THE RIVERSIDE COUNTY, STATE OF CALIFORNIA AS SHOWN IN DOCUMENT RECORDED DECEMBER 18, 2000 AS INSTRUMENT NO. 2001-501788, OFFICIAL RECORDS OF SAID RIVERSIDE COUNTY, EXCEPTING THAT PORTION MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTH-WESTERLY CORNER OF SAID PARCEL B, THENCE ALONG THE NORTHERLY LINE OF SAID PARCEL B SOUTH 89°29'49" EAST, A DISTANCE OF 169.02 FEET;

THENCE ALONG THE NORTH-EASTERLY LINE OF SAID PARCEL B SOUTH 70°06'22" EAST, A DISTANCE OF 2.23 FEET TO A LINE PARALLEL WITH AND 0.74 FEET SOUTHERLY OF THE NORTHERLY LINE OF SAID PARCEL B, SAID LINE BEING HALF WAY NORTH AND SOUTH BETWEEN THE EXISTING BUILDINGS;

THENCE ALONG NORTH 89°29'49" WEST, A DISTANCE OF 171.12 FEET TO A POINT ON THE WESTERLY LINE OF SAID PARCEL B, THENCE ALONG THE WESTERLY LINE OF SAID PARCEL B NORTH 00°30'11" EAST, A DISTANCE OF 0.74 FEET TO THE POINT OF BEGINNING.

AND TOGETHER WITH A PORTION OF PARCEL 11 OF PARCEL MAP 29068-1 IN SAID CITY OF PALM DESERT, COUNTY OF RIVERSIDE, AS SHOWN IN PLAT MAP BOOK 195, PAGE 27-28, RECORDS OF SAID COUNTY MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTH-WESTERLY CORNER OF LOT A OF SAID PARCEL MAP 29068-1,

THENCE ALONG A NORTHERLY PROLONGATION OF THE WESTERLY LINE OF SAID LOT A NORTH 00°30'11" EAST, A DISTANCE OF 2.23 FEET TO A POINT ON A LINE PARALLEL WITH AND 0.74 FEET NORTHEASTERLY OF THE SOUTH-WESTERLY LINE OF SAID PARCEL 11.

THENCE ALONG SAID PARALLEL LINE NORTH 70°06'22" WEST, A DISTANCE OF 69.77 FEET.

THENCE NORTH 89°29'49" WEST, A DISTANCE OF 15.06 FEET TO A POINT ON THE SOUTH-WESTERLY LINE OF SAID PARCEL 11,

THENCE ALONG THE SOUTH-WESTERLY LINE OF SAID PARCEL II SOUTH 70°06'22" EAST, A DISTANCE OF 85.73 FEET TO THE POINT OF BEGINNING.

NOTE: THIS COMPANY HAS PROVIDED SAID DESCRIPTION AS AN ACCOMMODATION FOR THE PURPOSE OF FACILITATING THIS REPORT PURSUANT TO THE SUBDIVISION MAP ACT OF THE STATE OF CALIFORNIA. SAID DESCRIPTION MAY NOT BE AN INSURABLE PARCEL, AND SHOULD NOT BE RELIED UPON TO CONVEY FINANCE OR ENCUMBER SAID LAND UNTIL APPROVED BY THE APPROPRIATE GOVERNING AGENCY.

APN: 626-410-046

**SCHEDULE "B" NOTE**

REFERENCE IS MADE TO THE TITLE REPORT ORDER NO. 92015681-920-046-CMS, GUARANTEE NO. CA-SFPC-IMP-81028-1-21-92015681, ISSUED BY COMMONWEALTH LAND TITLE, DATED AUGUST 6, 2021. ALL EASEMENTS NOTED WITHIN SAID TITLE REPORT AFFECTING THE IMMEDIATE AREA SURROUNDING THE LEASE HAVE BEEN PLOTTED.

- 1. PROPERTY TAXES WHICH ARE A LIEN NOT YET DUE AND PAYABLE, INCLUDING ANY ASSESSMENTS COLLECTED WITH TAXES TO BE LEVIED FOR THE FISCAL YEAR 2021-2022. (NOT A SURVEY MATTER - NOT SHOWN)
- 2. NOTE: PROPERTY TAXES, INCLUDING ANY PERSONAL PROPERTY TAXES AND ANY ASSESSMENTS COLLECTED WITH TAXES, ARE PAID FOR PROPRATION PURPOSES THE AMOUNTS WERE:  
TAX IDENTIFICATION NO.: 626-410-046  
FISCAL YEAR: 2020-2021  
1ST INSTALLMENT: \$11,013.12  
2ND INSTALLMENT: \$11,013.12  
EXEMPTION: 0.00  
CODE AREA: 019-224  
(NOT A SURVEY MATTER - NOT SHOWN)

- 3. SUPPLEMENTAL TAXES, INCLUDING ANY PERSONAL PROPERTY TAXES AND ANY ASSESSMENTS COLLECTED WITH TAXES, ASSESSED PURSUANT TO THE PROVISIONS OF CHAPTER 3.5 (COMMENCING WITH SECTION 75) OF THE REVENUE AND TAXATION CODE OF THE STATE OF CALIFORNIA, ARE AS FOLLOWS:  
TAX IDENTIFICATION NO.: 626-410-046  
FISCAL YEAR: 2019-2019  
1ST INSTALLMENT: \$2,757.49 PAID  
2ND INSTALLMENT: \$2,757.49 PAID  
CODE AREA: 019-224  
SUPPLEMENTAL BILL NO.: 201800257974  
(NOT A SURVEY MATTER - NOT SHOWN)

- 4. ANY LIENS OR OTHER ASSESSMENTS, BONDS, OR SPECIAL DISTRICT LIENS INCLUDING WITHIN LIMITATION, COMMUNITY FACILITY DISTRICTS, THAT ARISE BY REASON OF ANY LOCAL CITY, MUNICIPAL, OR COUNTY PROJECT OR SPECIAL DISTRICT. (NOT A SURVEY MATTER - NOT SHOWN)
- 5. THE LIEN OF SUPPLEMENTAL OR ESCAPED ASSESSMENTS OF PROPERTY TAXES, IF ANY, MADE PURSUANT TO THE PROVISIONS OF CHAPTER 3.5 (COMMENCING WITH SECTION 75) OR PART 2, CHAPTER 3, ARTICLES 3 AND 4, RESPECTIVELY, OF THE REVENUE AND TAXATION CODE OF THE STATE OF CALIFORNIA AS A RESULT OF THE TRANSFER OF TITLE TO THE VESSEE NAMED IN SCHEDULE A OR AS A RESULT OF CHANGES IN OWNERSHIP OR NEW CONSTRUCTION OCCURRING PRIOR TO DATE OF POLICY. (NOT A SURVEY MATTER - NOT SHOWN)

- 6. WATER RIGHTS, CLAIMS OR TITLE TO WATER, WHETHER OR NOT DISCLOSED BY THE PUBLIC RECORDS. (BLANKET IN NATURE)

- 7. EASEMENT(S) FOR THE PURPOSE(S) SHOWN BELOW AND RIGHTS INCIDENTAL THERETO, AS GRANTED TO COACHELLA VALLEY WATER DISTRICT  
ENTITLED: UNDERGROUND PIPELINE  
RECORDING DATE: JULY 13, 2000  
RECORDING NO. 84-152036, OFFICIAL RECORDS  
AFFECTS: A PORTION OF SAID LAND (DOES NOT AFFECT PARCEL)

- 8. RECORDS AS SHOWN ON THAT CERTAIN MAP/PLAT RECORDED NO. BOOK 142, PAGES 54 THROUGH 57, OF PARCEL MAPS WHICH AMONG OTHER THINGS RECITES NO BUILDINGS OBSTRUCTIONS OR ENCUMBRANCES BY LAND FILLS ARE ALLOWED. REFERENCE IS HEREBY MADE TO SAID DOCUMENT FOR FULL PARTICULARS. (NOT A SURVEY MATTER - NOT SHOWN)

- 9. RECORDS AS SHOWN ON THAT CERTAIN MAP/PLAT RECORDED NO. BOOK 196, PAGES B AND B OF PARCEL MAPS WHICH AMONG OTHER THINGS RECITES NO BUILDINGS OBSTRUCTIONS OR ENCUMBRANCES BY LAND FILLS ARE ALLOWED. REFERENCE IS HEREBY MADE TO SAID DOCUMENT FOR FULL PARTICULARS. (NOT A SURVEY MATTER - NOT SHOWN)

- 10. A NOTICE ENTITLED: ENGINEER'S REPORT FOR FISCAL YEAR 1990-1991 PURSUANT TO THE LANDSCAPING AND LIGHTING ACT OF 1972.  
FOR: LANDSCAPE MAINTENANCE ASSESSMENT DISTRICT NO. 85-1  
EXECUTED BY: COUNTY OF RIVERSIDE  
RECORDING DATE: AUGUST 2, 1990  
RECORDING NO. 90-288662, OFFICIAL RECORDS  
REFERENCE IS HEREBY MADE TO SAID DOCUMENT FOR FULL PARTICULARS. (NOT A SURVEY MATTER - NOT SHOWN)

- 11. COVENANTS, CONDITIONS AND RESTRICTIONS BUT OMITTING ANY COVENANTS OR RESTRICTIONS, IF ANY, INCLUDING BUT NOT LIMITED TO THOSE BASED UPON RACE, COLOR, RELIGION, SEX, GENDER, GENDER IDENTITY, GENDER EXPRESSION, SEXUAL ORIENTATION, MARITAL STATUS, NATIONAL ORIGIN, ANCESTRY, FAMILIAL STATUS, SOURCE OF INCOME, DISABILITY, VETERAN OR MILITARY STATUS, GENETIC INFORMATION, MEDICAL CONDITION, CITIZENSHIP, PRIMARY LANGUAGE, AND IMMIGRATION STATUS, AS SET FORTH IN APPLICABLE STATE OR FEDERAL LAWS, EXCEPT TO THE EXTENT THAT SAID COVENANT OR RESTRICTION IS PERMITTED BY APPLICABLE LAW, AS SET FORTH IN THE DOCUMENT OR SHALL NOT DEFEAT THE LIEN OF ANY MORTGAGE OR DEED OF TRUST MADE IN GOOD FAITH AND FOR VALUE.  
(ACCESS; UTILITY; WATER & DRAINAGE EASEMENTS, BLANKET IN NATURE - NOT SHOWN)

- 12. THE PROVISIONS OF SAID COVENANTS, CONDITIONS AND RESTRICTIONS WERE EXTENDED TO INCLUDE THE HEREIN DESCRIBED LAND BY AN INSTRUMENT  
RECORDING DATE: MAY 24, 2000  
RECORDING NO. 2000-197200, OFFICIAL RECORDS  
(NOT A SURVEY MATTER - NOT SHOWN)

- 13. MATTERS CONTAINED IN THAT CERTAIN DOCUMENT ENTITLED: COMMERCIAL DOMESTIC WATER AND/OR SANITATION INSTALLATION  
DATED: DECEMBER 17, 1999  
EXECUTED BY: COACHELLA VALLEY WATER DISTRICT AND SIXTH STREET PARTNERS I, LLC  
RECORDING DATE: JULY 13, 2000  
RECORDING NO. 2000-18130, OFFICIAL RECORDS  
REFERENCE IS HEREBY MADE TO SAID DOCUMENT FOR FULL PARTICULARS. (NOT A SURVEY MATTER - NOT SHOWN)

- 14. EASEMENT(S) FOR THE PURPOSE(S) SHOWN BELOW AND RIGHTS INCIDENTAL THERETO, AS GRANTED IN A DOCUMENT:  
GRANTED TO: COACHELLA VALLEY WATER DISTRICT, A PUBLIC AGENCY OF THE STATE OF CALIFORNIA  
PURPOSE: PUBLIC UTILITIES  
RECORDING DATE: MARCH 9, 2000  
RECORDING NO. 2000-88586, OFFICIAL RECORDS  
AFFECTS: A PORTION OF SAID LAND (AS SHOWN ON SURVEY)

**SCHEDULE "B" NOTE (CONTINUED)**

- 15. EASEMENT(S) FOR THE PURPOSE(S) SHOWN BELOW AND RIGHTS INCIDENTAL THERETO, AS GRANTED IN A DOCUMENT:  
GRANTED TO: GTE CALIFORNIA INCORPORATED, A CORPORATION  
PURPOSE: PUBLIC UTILITIES  
RECORDING DATE: APRIL 11, 2000  
RECORDING NO. 2000-133438, OFFICIAL RECORDS  
AFFECTS: A PORTION OF SAID LAND (EASEMENT UNDEFINED - NOT SHOWN)

- 16. EASEMENT(S) FOR THE PURPOSE(S) SHOWN BELOW AND RIGHTS INCIDENTAL THERETO, AS GRANTED TO: SOUTHERN CALIFORNIA EDISON COMPANY, A CORPORATION  
PURPOSE: PUBLIC UTILITIES  
RECORDING DATE: APRIL 21, 2000  
RECORDING NO. 2000-325927, OFFICIAL RECORDS  
AFFECTS: A PORTION OF SAID LAND (AS SHOWN ON SURVEY)

- 17. EASEMENT(S) FOR THE PURPOSE(S) SHOWN BELOW AND RIGHTS INCIDENTAL THERETO, AS GRANTED IN A DOCUMENT:  
GRANTED TO: COACHELLA VALLEY WATER DISTRICT, A PUBLIC AGENCY OF THE STATE OF CALIFORNIA  
PURPOSE: PUBLIC UTILITIES  
RECORDING DATE: SEPTEMBER 6, 2000  
RECORDING NO. 2000-550127, OFFICIAL RECORDS  
AFFECTS: A PORTION OF SAID LAND (UNDERLYING DOCUMENT NOT PROVIDED - NOT SHOWN)

- 18. MATTERS CONTAINED IN THAT CERTAIN DOCUMENT ENTITLED: COMMERCIAL DOMESTIC WATER AND/OR SANITATION INSTALLATION  
DATED: SEPTEMBER 13, 2000  
EXECUTED BY: COACHELLA VALLEY WATER DISTRICT AND WHITE ROCK INVESTMENTS, INC.  
RECORDING DATE: APRIL 12, 2001  
RECORDING NO. 2000-387974, OFFICIAL RECORDS  
REFERENCE IS HEREBY MADE TO SAID DOCUMENT FOR FULL PARTICULARS. (NOT A SURVEY MATTER - NOT SHOWN)

- 19. EASEMENT(S) FOR THE PURPOSE(S) SHOWN BELOW AND RIGHTS INCIDENTAL THERETO, AS GRANTED IN A DOCUMENT:  
GRANTED TO: SOUTHERN CALIFORNIA EDISON COMPANY, A CORPORATION  
PURPOSE: PUBLIC UTILITIES  
RECORDING DATE: APRIL 11, 2000  
RECORDING NO. 2000-133438, OFFICIAL RECORDS  
AFFECTS: A PORTION OF SAID LAND (DOES NOT AFFECT PARCEL)

- 20. MATTERS CONTAINED IN THAT CERTAIN DOCUMENT ENTITLED: LANDSCAPE MAINTENANCE AGREEMENT  
DATED: APRIL 12, 2001  
EXECUTED BY: CITY OF PALM DESERT, A MUNICIPAL CORPORATION AND WHITE ROCK INVESTMENTS, INC.  
RECORDING DATE: APRIL 12, 2001  
RECORDING NO. 2001-152022, OFFICIAL RECORDS  
REFERENCE IS HEREBY MADE TO SAID DOCUMENT FOR FULL PARTICULARS. (NOT A SURVEY MATTER - NOT SHOWN)

- 21. MATTERS CONTAINED IN THAT CERTAIN DOCUMENT ENTITLED: LANDSCAPE MAINTENANCE AGREEMENT  
DATED: MARCH 23, 2001  
EXECUTED BY: CITY OF PALM DESERT, A MUNICIPAL CORPORATION AND WHITE ROCK INVESTMENTS, INC.  
RECORDING DATE: APRIL 18, 2001  
RECORDING NO. 2001-165694, OFFICIAL RECORDS  
REFERENCE IS HEREBY MADE TO SAID DOCUMENT FOR FULL PARTICULARS. (NOT A SURVEY MATTER - NOT SHOWN)

- 22. MATTERS CONTAINED IN THAT CERTAIN DOCUMENT ENTITLED: COMMERCIAL DOMESTIC WATER AND/OR SANITATION INSTALLATION  
EXECUTED BY: COACHELLA VALLEY WATER DISTRICT AND SIXTH STREET I, LLC  
RECORDING DATE: JULY 13, 2000  
RECORDING NO. 2001-307893, OFFICIAL RECORDS  
REFERENCE IS HEREBY MADE TO SAID DOCUMENT FOR FULL PARTICULARS. (NOT A SURVEY MATTER - NOT SHOWN)

- 23. MATTERS CONTAINED IN THAT CERTAIN DOCUMENT ENTITLED: RESOLUTION OF THE BOARD OF DIRECTORS OF COACHELLA VALLEY WATER DISTRICT  
RESOLUTION NO. 2003-597450, OFFICIAL RECORDS  
RECORDING DATE: AUGUST 8, 2003  
RECORDING NO. 2003-597450, OFFICIAL RECORDS  
REFERENCE IS HEREBY MADE TO SAID DOCUMENT FOR FULL PARTICULARS. (NOT A SURVEY MATTER - NOT SHOWN)

- 24. SAID LAND IS LOCATED WITHIN THE BOUNDARIES OF THE ENERGY INDEPENDENCE PROGRAM IN ACCORDANCE WITH SECTION 5898.22 OF CHAPTER 39 OF PART 3 OF DIVISION 7 OF THE CALIFORNIA STREETS AND HIGHWAYS CODE AS SHOWN ON A MAP RECORDED OCTOBER 17, 2008 IN BOOK 195, PAGE 40 OF MAPS OF ASSESSMENT AND COMMUNITY FACILITIES DISTRICTS AND AS INSTRUMENT NO. 2008-589806, OFFICIAL RECORDS OF RIVERSIDE COUNTY. (NOT A SURVEY MATTER - NOT SHOWN)

- 25. COVENANTS, CONDITIONS AND RESTRICTIONS BUT OMITTING ANY COVENANTS OR RESTRICTIONS, IF ANY, INCLUDING BUT NOT LIMITED TO THOSE BASED UPON RACE, COLOR, RELIGION, SEX, GENDER, GENDER IDENTITY, GENDER EXPRESSION, SEXUAL ORIENTATION, MARITAL STATUS, NATIONAL ORIGIN, ANCESTRY, FAMILIAL STATUS, SOURCE OF INCOME, DISABILITY, VETERAN OR MILITARY STATUS, GENETIC INFORMATION, MEDICAL CONDITION, CITIZENSHIP, PRIMARY LANGUAGE, AND IMMIGRATION STATUS, AS SET FORTH IN APPLICABLE STATE OR FEDERAL LAWS, EXCEPT TO THE EXTENT THAT SAID COVENANT OR RESTRICTION IS PERMITTED BY APPLICABLE LAW, AS SET FORTH IN THE DOCUMENT OR SHALL NOT DEFEAT THE LIEN OF ANY MORTGAGE OR DEED OF TRUST MADE IN GOOD FAITH AND FOR VALUE.  
(INGRESS/EGRESS; UTILITIES, FIRE/EMERGENCY, ENCROACHMENT EASEMENTS, BLANKET IN NATURE - NOT SHOWN)

- 26. A DEED OF TRUST TO SECURE AN INDEBTEDNESS IN THE AMOUNT SHOWN BELOW, AMOUNT: \$93,750.00  
DATED: NOVEMBER 20, 2018  
TRUSTEES: TRUSTEES OF MARIO HERNANDEZ AND ROSE MARY HERNANDEZ, TRUSTEES OF THE HERNANDEZ FAMILY TRUST, DATED MAY 9, 2000  
TRUSTEE: ZIONS BANCORPORATION, N.A. DBA CALIFORNIA BANK AND TRUST BENEFICIARY: (N/A)  
RECORDING NO.: 2003  
RECORDING DATE: NOVEMBER 27, 2018  
RECORDING NO. 2018-046103, OFFICIAL RECORDS  
AFFECTS: THE HEREIN DESCRIBED LAND AND OTHER LAND. (NOT A SURVEY MATTER - NOT SHOWN)

- 27. AN ASSIGNMENT OF ALL THE MONEY DUE, OR TO BECOME DUE AS RENTAL, AS ADDITIONAL SECURITY FOR THE OBLIGATIONS SECURED BY DEED OF TRUST SHOWN AS ITEM NO. 26  
ASSIGNED TO: ZIONS BANCORPORATION, N.A. DBA CALIFORNIA BANK AND TRUST  
RECORDING DATE: NOVEMBER 27, 2018  
RECORDING NO. 2018-046107, OFFICIAL RECORDS  
AFFECTS: THE HEREIN DESCRIBED LAND AND OTHER LAND. (NOT A SURVEY MATTER - NOT SHOWN)

**SCHEDULE "B" NOTE (CONTINUED)**

- 28. SUBORDINATION, NONDISTURBANCE AND ATTORNEY AGREEMENT, AND THE TERMS AND CONDITIONS THEREOF:  
LENDER: ZIONS BANCORPORATION, N.A. DBA CALIFORNIA BANK AND TRUST  
TENANT: GLAZCON PRODUCTION, INC.  
LANDLORD: THE HERNANDEZ FAMILY TRUST ESTABLISHED MAY 9, 2000  
RECORDING DATE: NOVEMBER 27, 2018  
RECORDING NO. 2018-046103, OFFICIAL RECORDS  
THE PRESENT OWNERSHIP OF THE LEASEHOLD CREATED BY SAID LEASE AND OTHER MATTERS AFFECTING THE INTEREST OF THE LESSEE ARE NOT SHOWN HEREIN.  
AN AGREEMENT RECORDED NOVEMBER 27, 2018 AT 2018-046103, OFFICIAL RECORDS WHICH STATES THAT SAID LEASE HAS BEEN MADE SUBORDINATE TO THE DOCUMENT.  
ENTITLED: DEED OF TRUST  
RECORDING DATE: NOVEMBER 27, 2018  
RECORDING NO. 2018-046103, OFFICIAL RECORDS  
AFFECTS: THE HEREIN DESCRIBED LAND AND OTHER LAND. (NOT A SURVEY MATTER - NOT SHOWN)

- 29. A DEED OF TRUST TO SECURE AN INDEBTEDNESS IN THE AMOUNT SHOWN BELOW:  
AMOUNT: \$737,000.00  
DATED: NOVEMBER 30, 2018  
TRUSTOR/GRANTOR MARIO HERNANDEZ AND ROSE MARY HERNANDEZ, TRUSTEES OF THE HERNANDEZ FAMILY TRUST, DATED MAY 9, 2000  
TRUSTEE: ZIONS BANCORPORATION, N.A. DBA CALIFORNIA BANK AND TRUST BENEFICIARY: CDC SMALL BUSINESS FINANCE.  
LOAN NO.: NONE SHOWN  
RECORDING DATE: DECEMBER 7, 2018  
RECORDING NO. 2018-0477634, OFFICIAL RECORDS  
AFFECTS: THE HEREIN DESCRIBED LAND AND OTHER LAND. AN ASSIGNMENT OF THE BENEFICIAL INTEREST UNDER SAID DEED OF TRUST WHICH NAMES ASSIGNEE: UNITED STATES SMALL BUSINESS ADMINISTRATION  
LOAN NO.: NONE SHOWN  
RECORDING DATE: DECEMBER 7, 2018  
RECORDING NO. 2018-0477635, OFFICIAL RECORDS  
(NOT A SURVEY MATTER - NOT SHOWN)

- 30. SUBORDINATION, NONDISTURBANCE AND ATTORNEY AGREEMENT, AND THE TERMS AND CONDITIONS THEREOF:  
LENDER: CDC SMALL BUSINESS FINANCE  
TENANT: GLAZCON PRODUCTION, INC.  
LANDLORD: THE HERNANDEZ FAMILY TRUST ESTABLISHED MAY 9, 2000  
RECORDING DATE: DECEMBER 7, 2018  
RECORDING NO. 2018-0477637, OFFICIAL RECORDS  
THE PRESENT OWNERSHIP OF THE LEASEHOLD CREATED BY SAID LEASE AND OTHER MATTERS AFFECTING THE INTEREST OF THE LESSEE ARE NOT SHOWN HEREIN.  
AN AGREEMENT RECORDED DECEMBER 7, 2018 AT 2018-0477637, OFFICIAL RECORDS WHICH STATES THAT SAID LEASE HAS BEEN MADE SUBORDINATE TO THE DOCUMENT.  
ENTITLED: DEED OF TRUST  
RECORDING DATE: DECEMBER 7, 2018  
RECORDING NO. 2018-0477634, OFFICIAL RECORDS  
AFFECTS: THE HEREIN DESCRIBED LAND AND OTHER LAND. (NOT A SURVEY MATTER - NOT SHOWN)

- 31. MATTERS CONTAINED IN THAT CERTAIN DOCUMENT ENTITLED: THIRD PARTY LENDER AGREEMENT  
DATED: DECEMBER 5, 2018  
EXECUTED BY: ZIONS BANCORPORATION, N.A. DBA CALIFORNIA BANK AND TRUST AND CDC SMALL BUSINESS FINANCE  
RECORDING DATE: DECEMBER 7, 2018  
RECORDING NO. 2018-0477638, OFFICIAL RECORDS  
REFERENCE IS HEREBY MADE TO SAID DOCUMENT FOR FULL PARTICULARS. (NOT A SURVEY MATTER - NOT SHOWN)

- 32. ANY INVALIDITY OR DEFECT IN THE TITLE OF THE VESTEES IN THE EVENT THAT THE TRUST REFERRED TO HEREIN IS INVALID OR FAILS TO GRANT SUFFICIENT POWERS TO THE TRUSTEES OR IN THE EVENT THERE IS A LACK OF COMPLIANCE WITH THE TERMS AND PROVISIONS OF THE TRUST INSTRUMENT IF TITLE IS TO BE INSURED IN THE TRUSTEES OF A TRUST, (OR IF THEIR ACT IS TO BE INSURED), THIS COMPANY WILL REQUIRE A TRUST CERTIFICATION PURSUANT TO CALIFORNIA PROBATE CODE SECTION 181005. THE COMPANY RESERVES THE RIGHT TO ADD ADDITIONAL ITEMS OR MAKE FURTHER REQUIREMENTS AFTER REVIEW OF THE REQUESTED DOCUMENTATION. (THE EXCEPTION IS A STANDARD EXCEPTION AND NOT THE TYPE TO BE DEPICTED HEREON)

- 33. THE COMPANY WILL REQUIRE EITHER (A) A COMPLETE COPY OF THE TRUST AGREEMENT AND ANY AMENDMENTS THERETO CERTIFIED BY THE TRUSTEES TO BE A TRUE AND COMPLETE COPY WITH RESPECT TO THE HEREIN NAMED TRUST, OR (B) A CERTIFICATION, PURSUANT TO CALIFORNIA PROBATE CODE SECTION 181005, EXECUTED BY ALL OF THE CURRENT TRUSTEES (S) OF THE HEREIN NAMED TRUST, A FORM OF WHICH IS ATTACHED. NAME OF TRUST: THE HERNANDEZ FAMILY TRUST, DATED MAY 9, 2000 (THE EXCEPTION IS A STANDARD EXCEPTION AND NOT THE TYPE TO BE DEPICTED HEREON)

- 34. ANY RIGHTS OF THE PARTIES IN POSSESSION OF A PORTION OF, OR ALL OF, SAID LAND, WHICH RIGHTS ARE NOT DISCLOSED BY THE PUBLIC RECORDS, THE COMPANY WILL REQUIRE, FOR REVIEW, A FULL AND COMPLETE COPY OF ANY UNRECORDED AGREEMENT, CONTRACT, LICENSE AND/OR LEASE, TOGETHER WITH ALL SUPPLEMENTS, ASSIGNMENTS AND AMENDMENTS THERETO, BEFORE ISSUING ANY POLICY OF TITLE INSURANCE WITHOUT EXCEPTING THIS ITEM FROM COVERAGE. THE COMPANY RESERVES THE RIGHT TO EXCEPT ADDITIONAL ITEMS AND/OR MAKE ADDITIONAL REQUIREMENTS AFTER REVIEWING SAID DOCUMENTS. (THE EXCEPTION IS A STANDARD EXCEPTION AND NOT THE TYPE TO BE DEPICTED HEREON)

- 35. ANY RIGHTS OF THE PARTIES IN POSSESSION OF A PORTION OF, OR ALL OF, SAID LAND, WHICH RIGHTS ARE NOT DISCLOSED BY THE PUBLIC RECORDS, THE COMPANY WILL REQUIRE, FOR REVIEW, A FULL AND COMPLETE COPY OF ANY UNRECORDED AGREEMENT, CONTRACT, LICENSE AND/OR LEASE, TOGETHER WITH ALL SUPPLEMENTS, ASSIGNMENTS AND AMENDMENTS THERETO, BEFORE ISSUING ANY POLICY OF TITLE INSURANCE WITHOUT EXCEPTING THIS ITEM FROM COVERAGE. THE COMPANY RESERVES THE RIGHT TO EXCEPT ADDITIONAL ITEMS AND/OR MAKE ADDITIONAL REQUIREMENTS AFTER REVIEWING SAID DOCUMENTS. (THE EXCEPTION IS A STANDARD EXCEPTION AND NOT THE TYPE TO BE DEPICTED HEREON)

- 36. ANY RIGHTS OF THE PARTIES IN POSSESSION OF A PORTION OF, OR ALL OF, SAID LAND, WHICH RIGHTS ARE NOT DISCLOSED BY THE PUBLIC RECORDS, THE COMPANY WILL REQUIRE, FOR REVIEW, A FULL AND COMPLETE COPY OF ANY UNRECORDED AGREEMENT, CONTRACT, LICENSE AND/OR LEASE, TOGETHER WITH ALL SUPPLEMENTS, ASSIGNMENTS AND AMENDMENTS THERETO, BEFORE ISSUING ANY POLICY OF TITLE INSURANCE WITHOUT EXCEPTING THIS ITEM FROM COVERAGE. THE COMPANY RESERVES THE RIGHT TO EXCEPT ADDITIONAL ITEMS AND/OR MAKE ADDITIONAL REQUIREMENTS AFTER REVIEWING SAID DOCUMENTS. (THE EXCEPTION IS A STANDARD EXCEPTION AND NOT THE TYPE TO BE DEPICTED HEREON)

- 37. ANY RIGHTS OF THE PARTIES IN POSSESSION OF A PORTION OF, OR ALL OF, SAID LAND, WHICH RIGHTS ARE NOT DISCLOSED BY THE PUBLIC RECORDS, THE COMPANY WILL REQUIRE, FOR REVIEW, A FULL AND COMPLETE COPY OF ANY UNRECORDED AGREEMENT, CONTRACT, LICENSE AND/OR LEASE, TOGETHER WITH ALL SUPPLEMENTS, ASSIGNMENTS AND AMENDMENTS THERETO, BEFORE ISSUING ANY POLICY OF TITLE INSURANCE WITHOUT EXCEPTING THIS ITEM FROM COVERAGE. THE COMPANY RESERVES THE RIGHT TO EXCEPT ADDITIONAL ITEMS AND/OR MAKE ADDITIONAL REQUIREMENTS AFTER REVIEWING SAID DOCUMENTS. (THE EXCEPTION IS A STANDARD EXCEPTION AND NOT THE TYPE TO BE DEPICTED HEREON)

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- 40. ANY RIGHTS OF THE PARTIES IN POSSESSION OF A PORTION OF, OR ALL OF, SAID LAND, WHICH RIGHTS ARE NOT DISCLOSED BY THE PUBLIC RECORDS, THE COMPANY WILL REQUIRE, FOR REVIEW, A FULL AND COMPLETE COPY OF ANY UNRECORDED AGREEMENT, CONTRACT, LICENSE AND/OR LEASE, TOGETHER WITH ALL SUPPLEMENTS, ASSIGNMENTS AND AMENDMENTS THERETO, BEFORE ISSUING ANY POLICY OF TITLE INSURANCE WITHOUT EXCEPTING THIS ITEM FROM COVERAGE. THE COMPANY RESERVES THE RIGHT TO EXCEPT ADDITIONAL ITEMS AND/OR MAKE ADDITIONAL REQUIREMENTS AFTER REVIEWING SAID DOCUMENTS. (THE EXCEPTION IS A STANDARD EXCEPTION AND NOT THE TYPE TO BE DEPICTED HEREON)

- 41. ANY RIGHTS OF THE PARTIES IN POSSESSION OF A PORTION OF, OR ALL OF, SAID LAND, WHICH RIGHTS ARE NOT DISCLOSED BY THE PUBLIC RECORDS, THE COMPANY WILL REQUIRE, FOR REVIEW, A FULL AND COMPLETE COPY OF ANY UNRECORDED AGREEMENT, CONTRACT, LICENSE AND/OR LEASE, TOGETHER WITH ALL SUPPLEMENTS, ASSIGNMENTS AND AMENDMENTS THERETO, BEFORE ISSUING ANY POLICY OF TITLE INSURANCE WITHOUT EXCEPTING THIS ITEM FROM COVERAGE. THE COMPANY RESERVES THE RIGHT TO EXCEPT ADDITIONAL ITEMS AND/OR MAKE ADDITIONAL REQUIREMENTS AFTER REVIEWING SAID DOCUMENTS. (THE EXCEPTION IS A STANDARD EXCEPTION AND NOT THE TYPE TO BE DEPICTED HEREON)



1452 EDINGER AVENUE  
3RD FLOOR  
TUSTIN, CA 92780



ambit consulting  
428 MAIN STREET SUITE 205  
HUNTINGTON BEACH, CALIFORNIA 92648  
PH. (480) 659-4072



4400 E HIRALDIA AVE, SUITE 0  
ANAHEIM, CALIFORNIA 92807

3	01/03/22	UPDATE	(P0)
2	11/22/21	DESIGN	(R0)
1	09/06/21	TITLE WORK	(R0)
0	08/20/21	INITIAL ISSUE	(C0)
REV	DATE	DESCRIPTION	



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL SURVEYOR, TO ALTER THIS DOCUMENT.

CSL02434  
39750 GARAND LANE  
PALM DESERT, CA 92211

SHEET TITLE  
NOTES

SHEET NUMBER  
LS-2



THE INFORMATION CONTAINED IN THIS SET OF DRAWINGS IS PROPRIETARY & CONFIDENTIAL TO AT&T WIRELESS. ANY USE OR DISCLOSURE OTHER THAN AS IT RELATES TO AT&T WIRELESS IS STRICTLY PROHIBITED.



REV	DATE	DESCRIPTION
2	01/11/22	100% REDREV WITH LANDSCAPE
1	11/13/21	100% REDREV EQUIPMENT PLAN
0	10/22/21	100% ZONING DRAWINGS
0	09/28/21	80% ZONING DRAWINGS
A	08/25/21	80% ZONING DRAWINGS

**NOT TO BE USED FOR CONSTRUCTION**

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CSLO2434  
GARAND  
39750 GARAND LANE,  
PALM DESERT, CA 92211  
MONOPALM (INDOOR)

DRAWN BY: EMS      CHECKED BY: JS

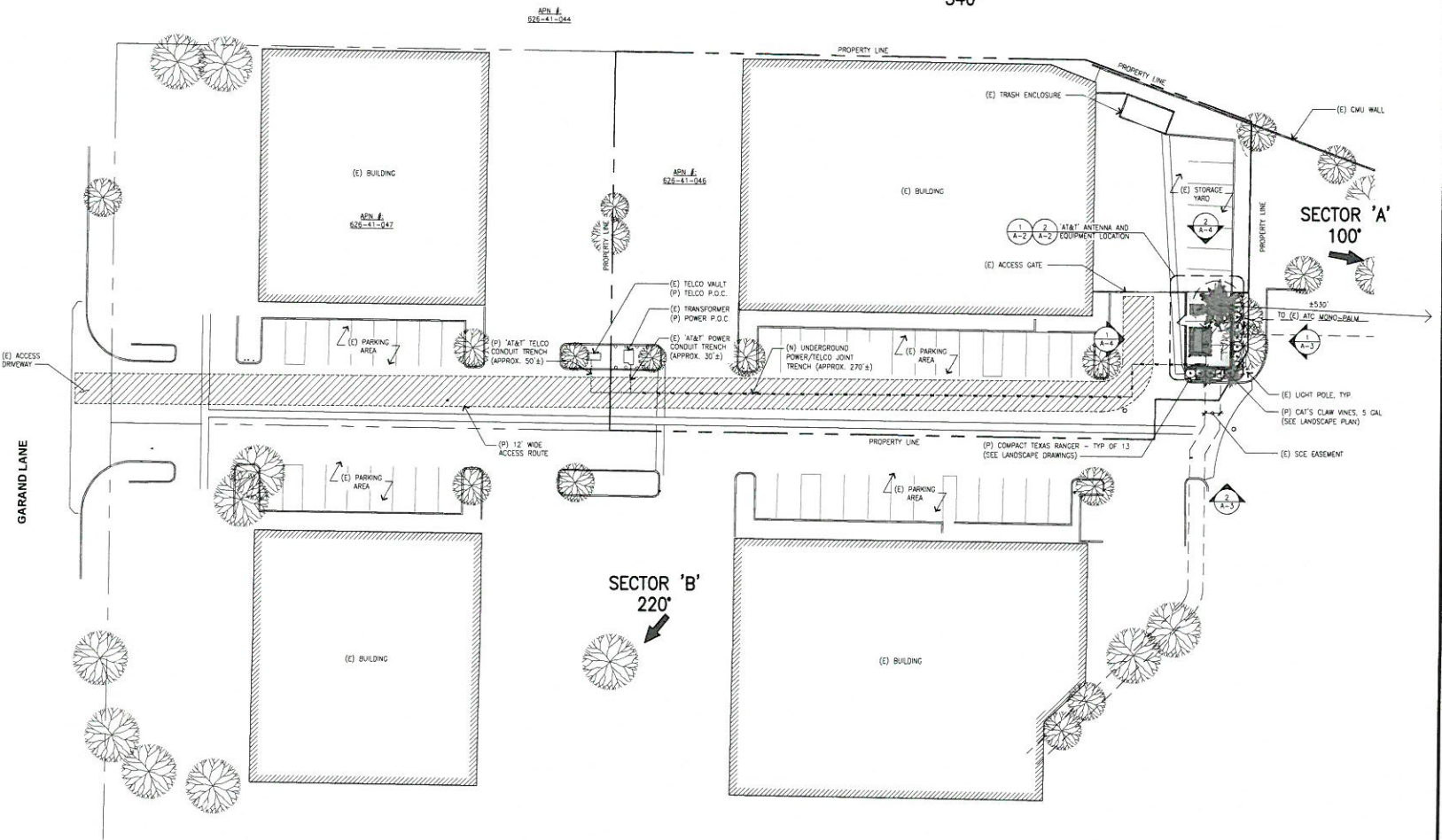
SHEET TITLE: **SITE PLAN**

SHEET NUMBER: **A-1**

SECTOR 'C'  
340'

SECTOR 'A'  
100'

SECTOR 'B'  
220'



NOTES:  
... UTILITY DESIGN IS PROPOSED AND BASED ON THE DESIGN WEST. FINAL UTILITY COORDINATION WILL DETERMINE THE POINT OF CONNECTION AND ROUTE.

**SITE PLAN**

SCALE: 1"=20'-0"







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**smartlink**  
 3300 IRVINE AVENUE, SUITE 300  
 NEWPORT BEACH, CA 92660  
 TEL: (949) 387-1265  
 FAX: (949) 387-1275



4430 E. MIRALOMA AVE., SUITE D  
 ANAHEIM, CALIFORNIA 92807

REV	DATE	DESCRIPTION
2	01/11/22	100% REVISED WITH LANDSCAPE
1	11/12/21	100% REVISED EQUIPMENT PLAN
0	10/22/21	100% ZONING DRAWINGS
8	09/28/21	90% ZONING DRAWINGS
A	08/25/21	90% ZONING DRAWINGS

**NOT TO BE USED FOR CONSTRUCTION**

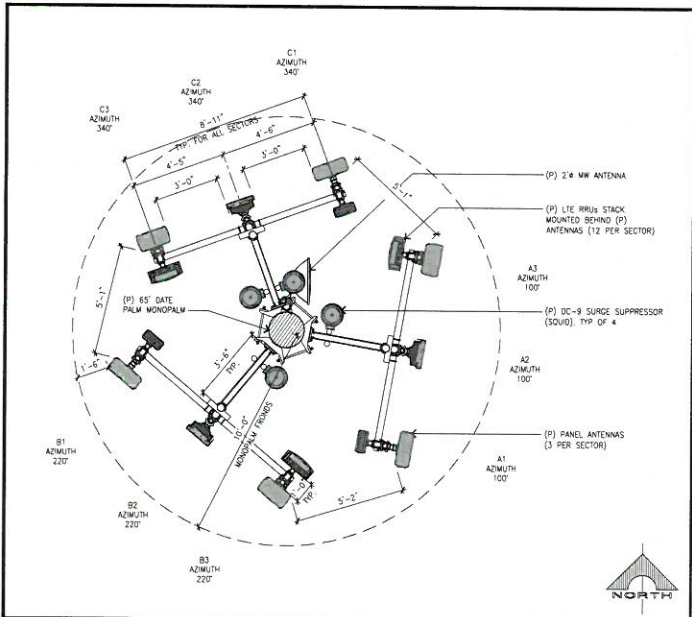
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**CSL02434 GARAND**  
 39750 GARAND LANE,  
 PALM DESERT, CA 92211  
 MONOPALM (INDOOR)

DRAWN BY: EMS  
 CHECKED BY: JS

SHEET TITLE:  
 LEASE AREA/ANTENNA PLAN  
 AND ANTENNA/RRU SCHEDULE

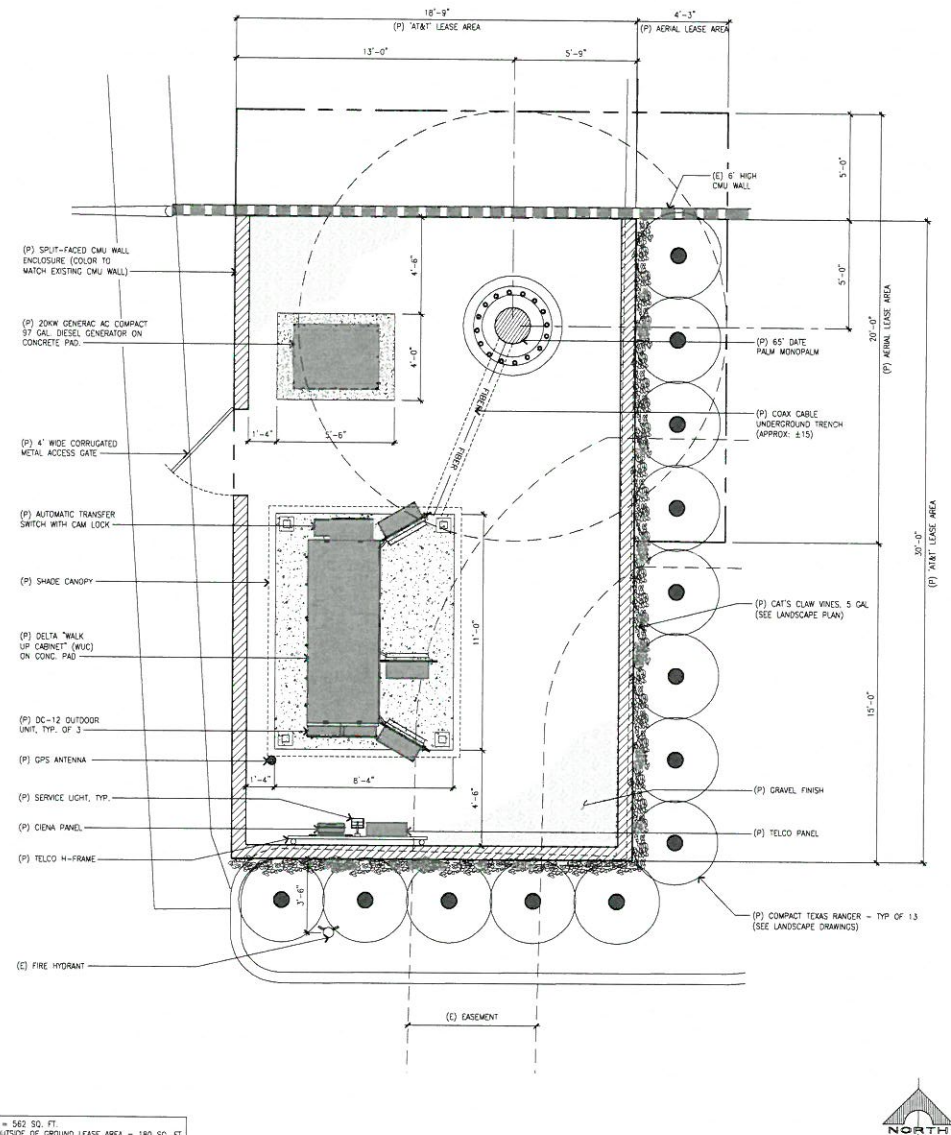
SHEET NUMBER:  
**A-2**



PROPOSED ANTENNA AND TRANSMISSION CABLE REQUIREMENTS								
SECTOR	PROPOSED TECHNOLOGY	ANTENNA		RAD CENTER	TRANSMISSION LINES (LENGTH FT +/-)			
		ARR/HKX/B-PORT	SIZE (4", 6", 8")		JUMPER	DC CABLE (AWG #)		
A1-A3 COMM. SECTOR	A1	LTE	COMMODORE NNA-45C-R6-V3	8"	100'	56'-0"	<12	+/- 70'
	A2	LTE	AIR649-N770	2'-3"	100'	58'-9"	<12	+/- 70'
	A3	LTE	SURTEL S08612-2	8"	100'	56'-0"	<12	+/- 70'
B1-B3 RRU SECTOR	B1	LTE	COMMODORE NNA-45C-R6-V3	8"	220'	56'-0"	<12	+/- 70'
	B2	LTE	AIR649-N770	2'-3"	220'	58'-9"	<12	+/- 70'
	B3	LTE	SURTEL S08612-2	8"	220'	56'-0"	<12	+/- 70'
C1-C3 COMM. SECTOR	C1	LTE	COMMODORE NNA-45C-R6-V3	8"	340'	56'-0"	<12	+/- 70'
	C2	LTE	AIR649-N770	2'-3"	340'	58'-9"	<12	+/- 70'
	C3	LTE	SURTEL S08612-2	8"	340'	56'-0"	<12	+/- 70'

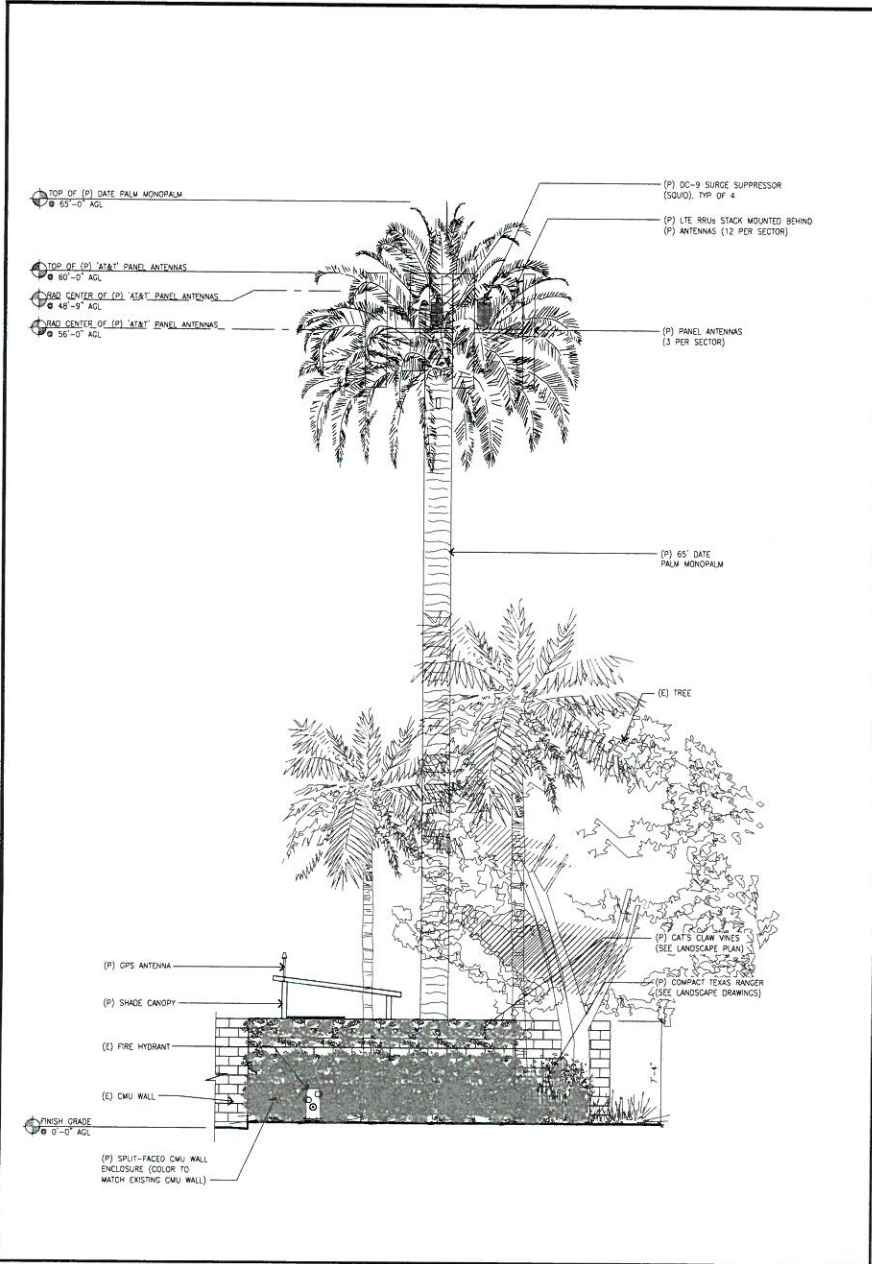
REWRITE RADIO UNITS (RRUS)					
SECTOR	RRU UP OR DOWN	RRU COUNT	RRU LOCATION (DISTANCE FROM ANTENNA)	MINIMUM CLEARANCES ABOVE BELOW SIDES	
A1-A3 RRU SECTOR	A1	RRUS 4449 B5/B12	1	<12'	18" 8" 8"
	A1	RRUS 8843 B25/B66A	1	<12'	18" 8" 8"
	A2	AIR649 N770	1	<12'	18" 8" 8"
B1-B3 RRU SECTOR	B1	RRUS 4449 B5/B12	1	<12'	18" 8" 8"
	B1	RRUS 8843 B25/B66A	1	<12'	18" 8" 8"
	B2	AIR649 N770	1	<12'	18" 8" 8"
C1-C3 COMM. SECTOR	C1	RRUS 4449 B5/B12	1	<12'	18" 8" 8"
	C1	RRUS 8843 B25/B66A	1	<12'	18" 8" 8"
	C2	AIR649 N770	1	<12'	18" 8" 8"
C3	C3	RRUS 4478 B14	1	<12'	18" 8" 8"
	C3	RRUS 4415 B30	1	<12'	18" 8" 8"
	C3	RRUS E2 B29	1	<12'	18" 8" 8"

GROUND LEASE AREA = 562 SQ. FT.  
 AERIAL LEASE AREA OUTSIDE OF GROUND LEASE AREA = 180 SQ. FT.



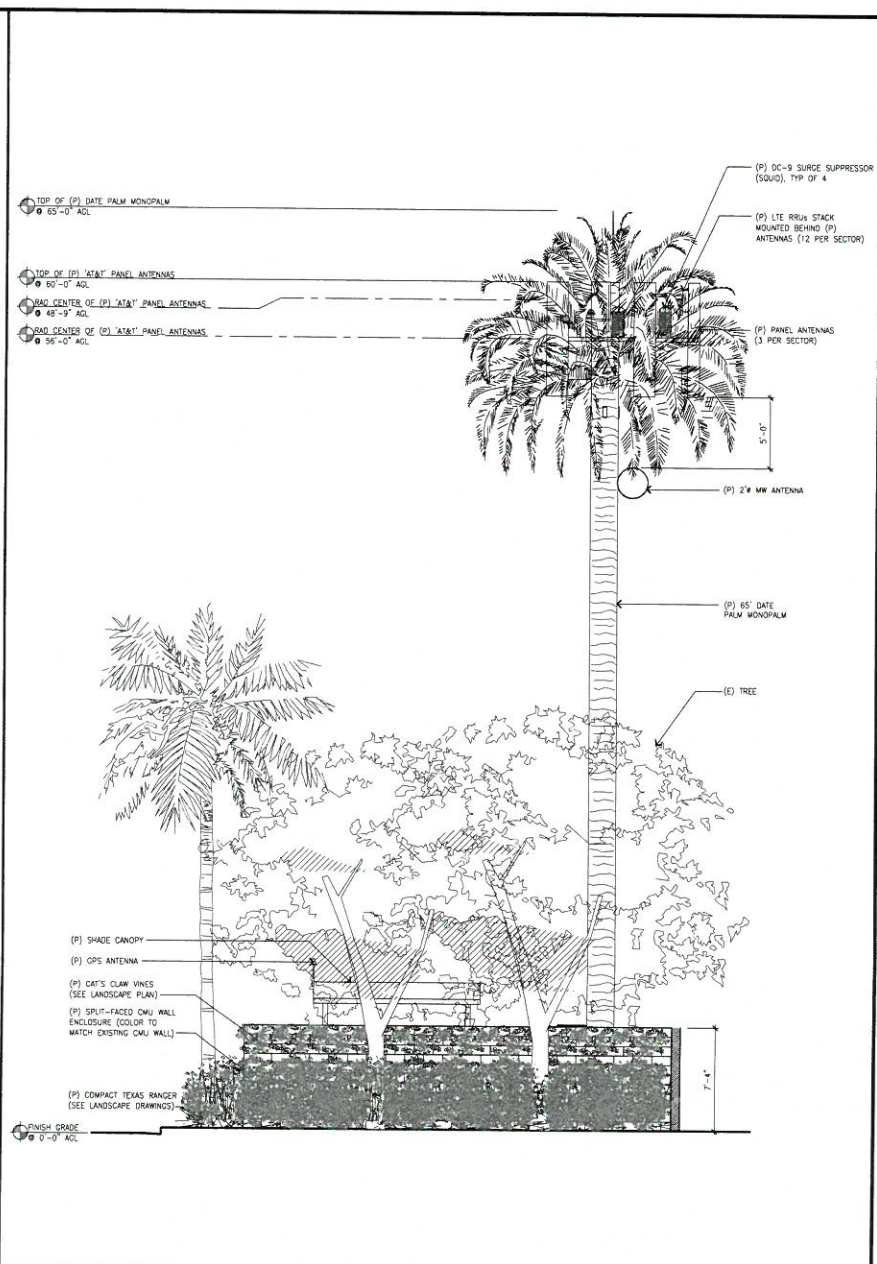
**ANTENNA AND RRU SCHEDULE**  
 SCALE: 3/8"=1'-0"

**LEASE AREA PLAN**  
 SCALE: 3/8"=1'-0"



**SOUTH ELEVATION**

SCALE: 1/4"=1'-0" **2**



**EAST ELEVATION**

SCALE: 1/4"=1'-0" **1**



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REV	DATE	DESCRIPTION
2	01/11/22	100% RFI'S WITH LANDSCAPE
1	11/13/21	100% RFI'S EQUIPMENT PLAN
0	10/22/21	100% ZONING DRAWINGS
B	09/28/21	90% ZONING DRAWINGS
A	08/25/21	90% ZONING DRAWINGS

**NOT TO BE USED FOR CONSTRUCTION**

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT.

**CSL02434 GARAND 39750 GARAND LANE, PALM DESERT, CA 92211 MONOPALM (INDOOR)**

DRAWN BY: EMS CHECKED BY: JS

SHEET TITLE: **ELEVATIONS**

SHEET NUMBER: **A-3**



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4430 E. MIRALOMA AVE, SUITE D  
ANAHEIM, CALIFORNIA 92807

REV	DATE	DESCRIPTION
2	01/11/22	100% REVISION WITH LANDSCAPE
1	11/12/21	100% REVISION EQUIPMENT PLAN
0	10/22/21	100% DESIGN DRAWINGS
0	09/29/21	90% DESIGN DRAWINGS
1	08/25/21	30% DESIGN DRAWINGS

**NOT TO BE USED FOR CONSTRUCTION**

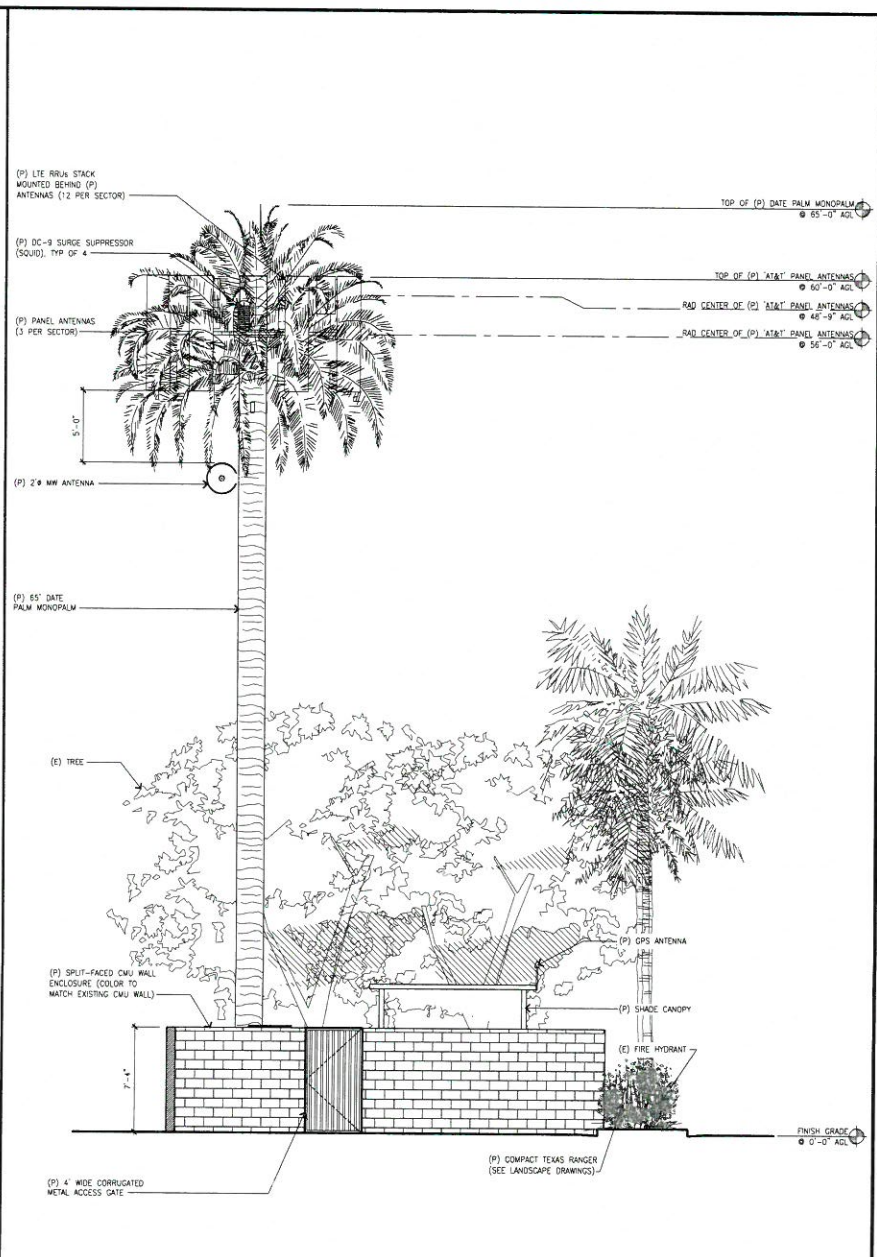
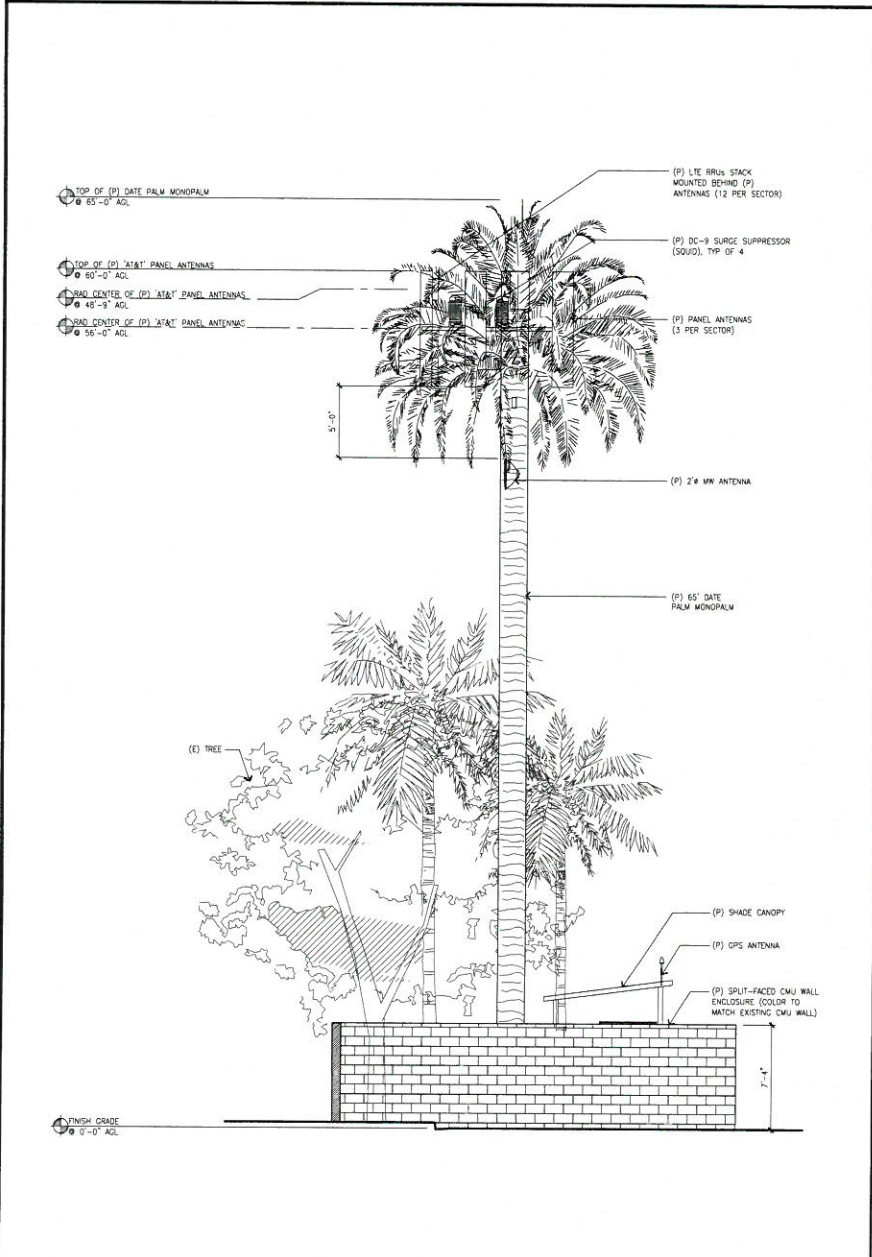
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CSL02434  
GARAND  
39750 GARAND LANE,  
PALM DESERT, CA 92211  
MONOPALM (INDOOR)

DRAWN BY: EMS      CHECKED BY: JS

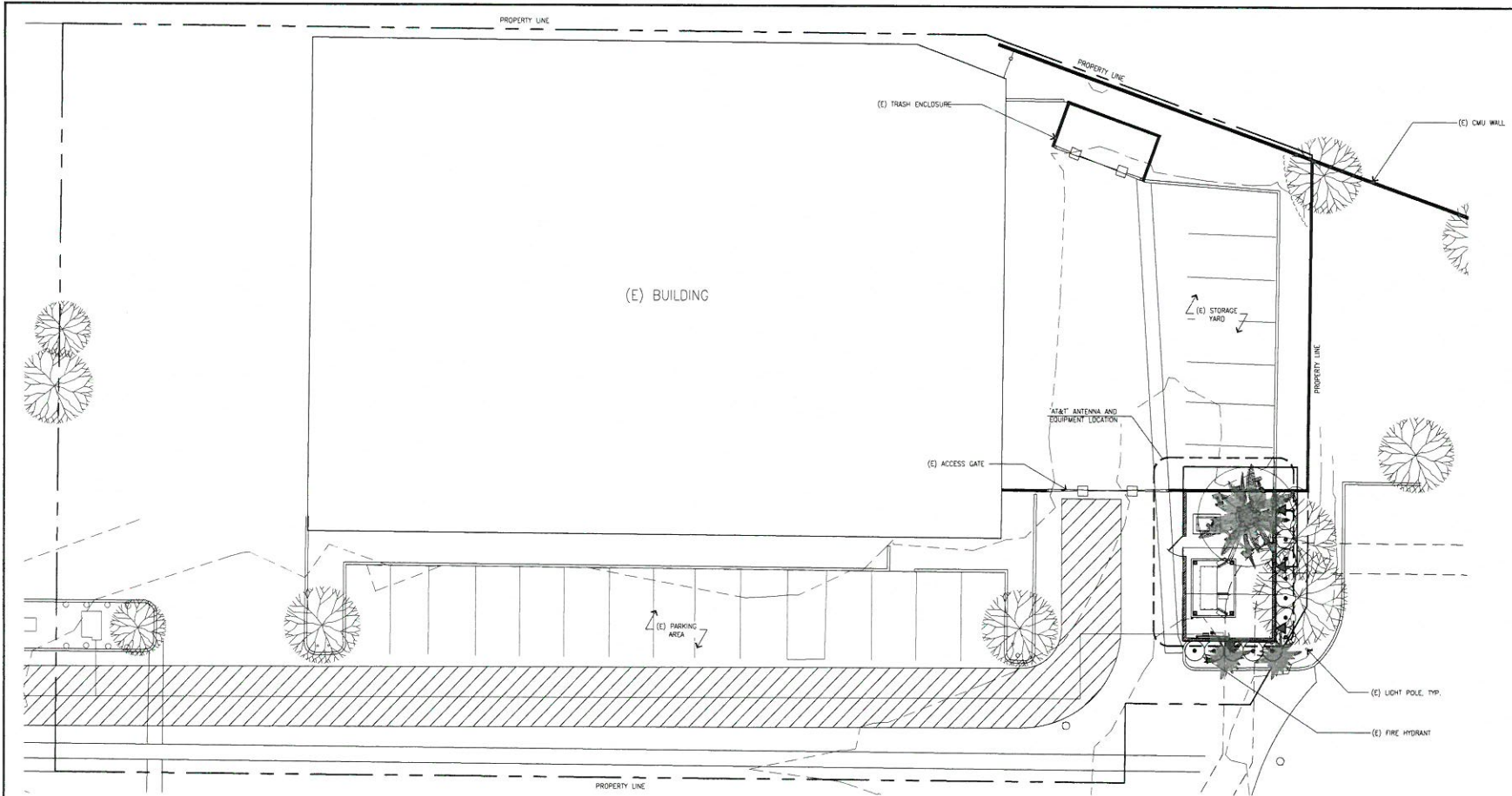
SHEET TITLE:  
ELEVATIONS

SHEET NUMBER:  
A-4



**NORTH ELEVATION**      SCALE: 1/4"=1'-0"      2

**WEST ELEVATION**      SCALE: 1/4"=1'-0"      1



**PLANT LEGEND**

SYM.	BOTANICAL NAME	COMMON NAME	SIZE	QTY.	REMARKS	WATER USE
<b>SHRUBS</b>						
●	LEUCOPHYLLUM FRUTICOSUM	COMPACT TEXAS RANGER	5 GAL.	3	PER PLAN	LOW
<b>VINES</b>						
▼	MACFADYENA UNGUIS-CATI	CAT'S CLAW VINE	5 GAL.	4	STAKED TO WALL	LOW

**PLANTING NOTE**

1. WORK WILL BE DONE PER CITY OF PALM DESERT AND COACHELLA VALLEY WATER DISTRICT GUIDELINES.
2. REPLACE DEAD OR MISSING GROUNDCOVER DAMAGED BY CONSTRUCTION OR NEGLECT.
3. PROPOSED PLANTING AND IRRIGATION SHALL BE MAINTAINED BY PRIVATE OWNER.
4. ALL PLANTING AREAS TO BE PROVIDED WITH 3" MIN. LAYER OF DECOMPOSED GRANITE MATCH EXISTING.
5. AUTOMATIC IRRIGATION TO BE SUBGRADE TREE BUBBLERS (2 PER PALM AND DRIP EMITTERS FOR SHAUBS. IRRIGATION SHALL PROVIDE 100% COVERAGE WITH NO OVERSPRAY OR RUN OFF. IRRIGATION TO INCLUDE REMOTE CONTROL VALVES, MIN. 3/4" PVC TRIPLE QUICK COUPLING VALVE BALL VALVE AND AUTOMATIC IRRIGATION CONTROLLER WITH PERMANENT POWER SOURCE.
6. PROPOSED PLANTING AND IRRIGATION SHALL BE MAINTAINED BY PRIVATE OWNER.



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REV	DATE	DESCRIPTION
1	1/3/2022	ISSUE LANDSCAPE ZONING DRAWINGS
0	08/27/21	ISSUE ZONING DRAWINGS
A	08/19/21	ISSUE ZONING DRAWINGS



**RJCLA**  
Landscape Architects  
11 Villanova  
Laguna Hills, CA 92653  
(949) 661-9358  
rjcla@rcglobe.com

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CSL02434  
GARAND  
39750 GARAND LANE,  
PALM DESERT, CA 92211  
MONOPALM (INDOOR)

DRAWN BY: mdm  
CHECKED BY: RJC

SHEET TITLE:  
PRELIMINARY  
LANDSCAPE PLAN

SHEET NUMBER:  
L-1

**PRELIMINARY LANDSCAPE PLAN**

SCALE: 1"=20'-0"  
10' 20' 40'



**NOTICE OF PUBLIC HEARING**  
**RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**  
[www.rcaluc.org](http://www.rcaluc.org)

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. **Information on how to participate in the hearing will be available on the ALUC website at [www.rcaluc.org](http://www.rcaluc.org).** The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893.**

The City of Palm Desert Planning Department should be contacted on non-ALUC issues. For more information please contact City of Palm Desert Planner Nick Melloni at (760) 776-6479.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website [www.rcaluc.org](http://www.rcaluc.org). Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to [prull@rivco.org](mailto:prull@rivco.org). Individuals with disabilities requiring reasonable modifications or accommodations, please contact Barbara Santos at (951) 955-5132.

**PLACE OF HEARING:**                    **Riverside County Administration Center  
4080 Lemon Street, 1<sup>st</sup> Floor Board Chambers  
Riverside California**

**DATE OF HEARING:**                **May 12, 2022**

**TIME OF HEARING:**                **9:30 A.M.**

**CASE DESCRIPTION:**

ZAP1086BD22 – AT&T (Representative: Smartlink Group) – City of Palm Desert Planning Department Case No. CUP22-0001 (Conditional Use Permit). A proposal to establish a 65-foot-tall mono-palm tree wireless communications facility with a 562 square foot equipment shelter on 0.81 acres, located at 39750 Garand Lane (Airport Compatibility Zone C of the Bermuda Dunes Airport Influence Area)



# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

Bermude Dunes, Zone C

## APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP1086BD22 DATE SUBMITTED: 03/17/2022

### APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

Applicant	<u>New Cingular Wireless dba AT&amp;T</u>	Phone Number	
Mailing Address	<u>1452 Edinger Ave, Tustin, CA 92780</u>	Email	<u>Same below</u>

Representative	<u>Chris Doheny from Smartlink</u>	Phone Number	<u>619-994-8528</u>
Mailing Address	<u>3300 Irvine Ave Suite 300</u>	Email	<u>chris.doheny@smartlinkgroup.com</u>
	<u>Newport Beach, CA 92660</u>		

Property Owner	<u>Hernandez Family Trust/Mario Hernandez</u>	Phone Number	<u>760-535-4976</u>
Mailing Address	<u>39750 Garand Lane, Palm Desert, CA 92211</u>	Email	<u>marioh@glazcon.com</u>

*John Rubio - rodriguez@smartlinkgroup.com*

### LOCAL JURISDICTION AGENCY

Local Agency Name	<u>City of Palm Desert</u>	Phone Number	<u>760-776-6479</u>
Staff Contact	<u>Nick Melloni</u>	Email	<u>nmelloni@cityofpalmdesert.org</u>
Mailing Address	<u>73-510 Fred Waring Dr., Palm Desert, CA 92260</u>	Case Type	<u>Conditional Use Permit</u>
Local Agency Project No	<u>CUP 22-0001</u>	<input type="checkbox"/>	General Plan / Specific Plan Amendment
		<input type="checkbox"/>	Zoning Ordinance Amendment
		<input type="checkbox"/>	Subdivision Parcel Map / Tentative Tract
		<input type="checkbox"/>	Use Permit
		<input checked="" type="checkbox"/>	Site Plan Review/Plot Plan
		<input type="checkbox"/>	Other

### PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address	<u>39750 Garand Land, Palm Desert, CA 92211</u>		
Assessor's Parcel No.	<u>626-410-046</u>	Gross Parcel Size	<u>0.81 acres</u>
Subdivision Name		Nearest Airport and distance from Airport	<u>3 Miles</u>
Lot Number			

### PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe)	<u>Currently the property is used for commercial use Industrial Building. The project is to build a new AT&amp;T cell site with an overall height of 65ft disguised a palm tree</u>
------------------------------	---

Proposed Land Use (describe)	The project is to build a new AT&T cell site with an overall height of 65ft disguised a palm tree, with supporting equipment and cabinets totaling not more than 575 SF	
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)	N/a
For Other Land Uses (See Appendix C)	Hours of Operation	N/a - unmanned facility
	Number of People on Site	Maximum Number
	Method of Calculation	
Height Data	Site Elevation (above mean sea level)	112.5 ft.
	Height of buildings or structures (from the ground)	65 ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
	If yes, describe	AT&T facilities are designed in compliance with airport requirements and should not have any interference with aircraft flight.

- A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:**
1. . . . . Completed ALUC Application Form
  1. . . . . ALUC fee payment
  1. . . . . Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
  1. . . . . Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
  1. . . . . CD with digital files of the plans (pdf)
  1. . . . . Vicinity Map (8.5x11)
  1. . . . . Detailed project description
  1. . . . . Local jurisdiction project transmittal
  3. . . . . Gummed address labels for applicant/representative/property owner/local jurisdiction planner
  3. . . . . Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. **(Only required if the project is scheduled for a public hearing Commission meeting)**

# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

## STAFF REPORT

**AGENDA ITEM:** 3.2

**HEARING DATE:** May 12, 2022

**CASE NUMBER:** ZAP1117FV22 – HD Commercial Ventures, Inc  
(Representative: Storm Entitlement Permitting PM, LLC)

**APPROVING JURISDICTION:** County of Riverside

**JURISDICTION CASE NO:** PPT210140 (Plot Plan)

**LAND USE PLAN:** 2007 French Valley Airport Land Use Compatibility Plan, as amended in 2011

Airport Influence Area: French Valley Airport

Land Use Policy: Compatibility Zone C

Noise Levels : Below 55 CNEL from aircraft noise

**MAJOR ISSUES:** None

**RECOMMENDATION:** Staff recommends that the Commission find the proposed Plot Plan CONSISTENT with the 2007 French Valley Airport Land Use Compatibility Plan, as amended in 2011, subject to the conditions included herein.

**PROJECT DESCRIPTION:** A proposal to construct two industrial buildings totaling 173,653 square feet with mezzanines on 11.30 acres.

**PROJECT LOCATION:** The proposed project is located northerly of Murrieta Hot Springs Road, southerly of Technology Drive, westerly of Sky Canyon Drive, and easterly of Winchester Road, approximately 3,608 feet southwesterly of the southerly terminus of Runway 18-36 at French Valley Airport.

### **BACKGROUND:**

Non-Residential Average Intensity: Pursuant to the French Valley Airport Land Use Compatibility Plan, the project site is located within Compatibility Zone C of the French Valley Airport Influence Area. Within Compatibility Zone C of the French Valley Airport Influence Area, additional Compatibility Policy 2.3 limits average acre intensity to 80 people per acre.

Pursuant to Appendix C, Table C-1 of the Riverside County Airport Land Use Compatibility Plan, the following rate was used to calculate projected occupancy for the proposed building:



- Manufacturing area- 1 person per 200 square feet, and
- Warehouse area – 1 person per 500 square feet, and
- Office area – 1 person per 200 square feet.

The project proposes to construct 2 manufacturing warehouse buildings with mezzanines on totaling 173,653 square feet (on one parcel), accommodating an occupancy of 635 people, resulting in an average intensity of 56 people per average acre, consistent with zone C criterion of 80 people per average acre. An individual building analysis is included below:

- Building A includes 22,035 square feet of manufacturing area, 36,058 square feet of warehouse area, 15,221 square feet of first floor office area, and 6,814 square feet of second floor office mezzanines, accommodating an occupancy of 293 people resulting in an average intensity of 26 people per acre.
- Building B includes 25,720 square feet of manufacturing area, 42,086 square feet of warehouse area, 18,906 of first floor office area, and 6,814 square feet of second floor office mezzanine area, accommodating an occupancy of 342 people resulting in an average intensity of 30 people per acre.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per standard vehicle). Based on the number of vehicle parking spaces provided (273), the total occupancy would be estimated at 410 persons. This results in an average intensity of 36 people per acre, which is consistent with the Zone C average intensity criterion of 80.

Non-Residential Single-Acre Intensity: Pursuant to the French Valley Airport Land Use Compatibility Plan, the project site is located within Compatibility Zone C, where Additional Compatibility Policy 2.3 restricts single acre intensity to a maximum of 160 persons.

Based on the site plan provided and the occupancies as previously noted. The maximum single-acre area for each of the buildings are as follows:

- Building A: 11,445 square feet of manufacturing area. 16,221 square feet of warehouse area, 3,470 square feet of first floor office area and 3,435 square feet of second floor office mezzanines, resulting in a single acre intensity of 123 people, consistent with Zone C single acre criterion of 160 people per single acre.
- Building B: 12,346 square feet of manufacturing area, 17,374 square feet of warehouse area, 7,826 square feet of first floor office area, and 3,435 square feet of second floor office mezzanine area, resulting in a single acre intensity of 152 people, consistent with Zone C single acre criterion of 160 people per single acre.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zone C.

Noise: The French Valley Airport Land Use Compatibility Plan depicts the site as being located below the 55 CNEL contour range from aircraft noise. Therefore, special measures to mitigate aircraft-generated noise would not be required.

Part 77: The elevation of Runway 18-36 at its southerly terminus is 1,347 feet above mean sea level. At a distance of approximately 3,608 feet from the runway to the site, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,383 feet AMSL. The maximum finished floor elevation is 1,206 feet AMSL. With a maximum building height of 40 feet, the top point elevation would be 1,246 feet. Therefore, review of the building for height/elevation reasons by the FAA Obstruction Evaluation Service (FAAOES) is not required.

Open Area: Pursuant to the French Valley Airport Land Use Compatibility Plan, the project site is located within Compatibility Zone C. The Compatibility Plan requires projects 10 acres or larger to designate 20% of project area as ALUC qualifying open area that could potentially serve as emergency landing areas.

Based on the project size (11.30 acres) located within Compatibility Zone C, the project is required to provide a minimum 2.26 acres of open area consistent with ALUC open area criteria. The applicant has provided a total of 2.26 acres of open area within the drive aisles and parking areas. These areas are conditioned to maintain a minimum shape of 75 feet in width and 300 feet in length, and shall be kept obstacle and obstruction free per ALUC open area definition (no objects greater than four feet in height with a diameter of four inches or greater).

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33C). The project is located 3,608 feet from the runway, and therefore would be subject to the above requirement.

The project includes a 24,548 square foot bioretention basin. Bioretention basins are not recommended in the vicinity of airports due to the potential that such areas could provide food, water, and shelter for hazardous wildlife. Pursuant to the study "Wildlife Hazard Management at Riverside County Airports: Background and Policy", October 2018, by Mead & Hunt, which is the basis of the brochure titled "Airports, Wildlife and Stormwater Management", such limited basins are permissible with the appropriate criteria: the basin is used in conjunction with appropriate landscaping for such uses as adjacent to structures, parking islands, medians, site entrances, planter boxes, and vegetation is selected carefully so as not to provide food, shelter, nesting, roosting, or water for wildlife. The project has been conditioned to be consistent with the basin criteria (as well as providing 48-hour draw down of the basin).

**CONDITIONS:**

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an

initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.

- (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, outdoor production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators).
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Children's schools, day care centers, libraries, hospitals, nursing homes, places of worship, buildings with more than three aboveground habitable floors.
  - (f) Highly noise-sensitive outdoor nonresidential uses.
  - (g) Any use which results in a hazard to flight, including physical (e.g., tall objects), visual, and electronic forms of interference with the safety of aircraft operations.
3. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property and be recorded as a deed notice.
4. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the stormwater basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at [RCALUC.ORG](http://RCALUC.ORG) which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name,

telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

5. This project has been evaluated as consisting of 47,754 square feet of manufacturing area, 78,144 square feet of warehouse area and 47,755 square feet of office area. Any increase in building area, change in use to any higher intensity use, change in building location, or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.
6. At least 2.26 acres of ALUC-eligible open areas (at least 75 feet in width and 300 feet in length), as depicted on the Open Space exhibit, shall be kept obstacle and obstruction free per ALUC open area definition (no objects greater than four feet in height with a diameter of four inches or greater).
7. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission and Riverside County – Aviation Division as owner and operator of French Valley Airport. In the event of any reasonable complaint about glare related to aircraft operations, the applicant shall agree to such specific mitigation measures as determined or requested by Riverside County Aviation Division.

X:\AIRPORT CASE FILES\French Valley\ZAP1117FV22\ZAP1117FV22sr.doc

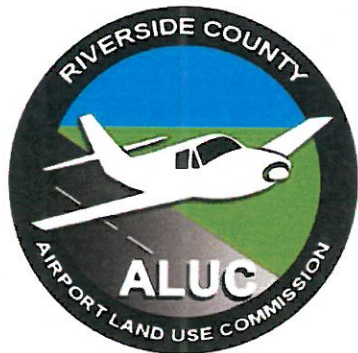
# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

# NOTICE

**THERE IS AN AIRPORT NEARBY.  
THIS STORM WATER BASIN IS DESIGNED TO HOLD  
STORM WATER FOR ONLY 48 HOURS AND  
NOT TO ATTRACT BIRDS**

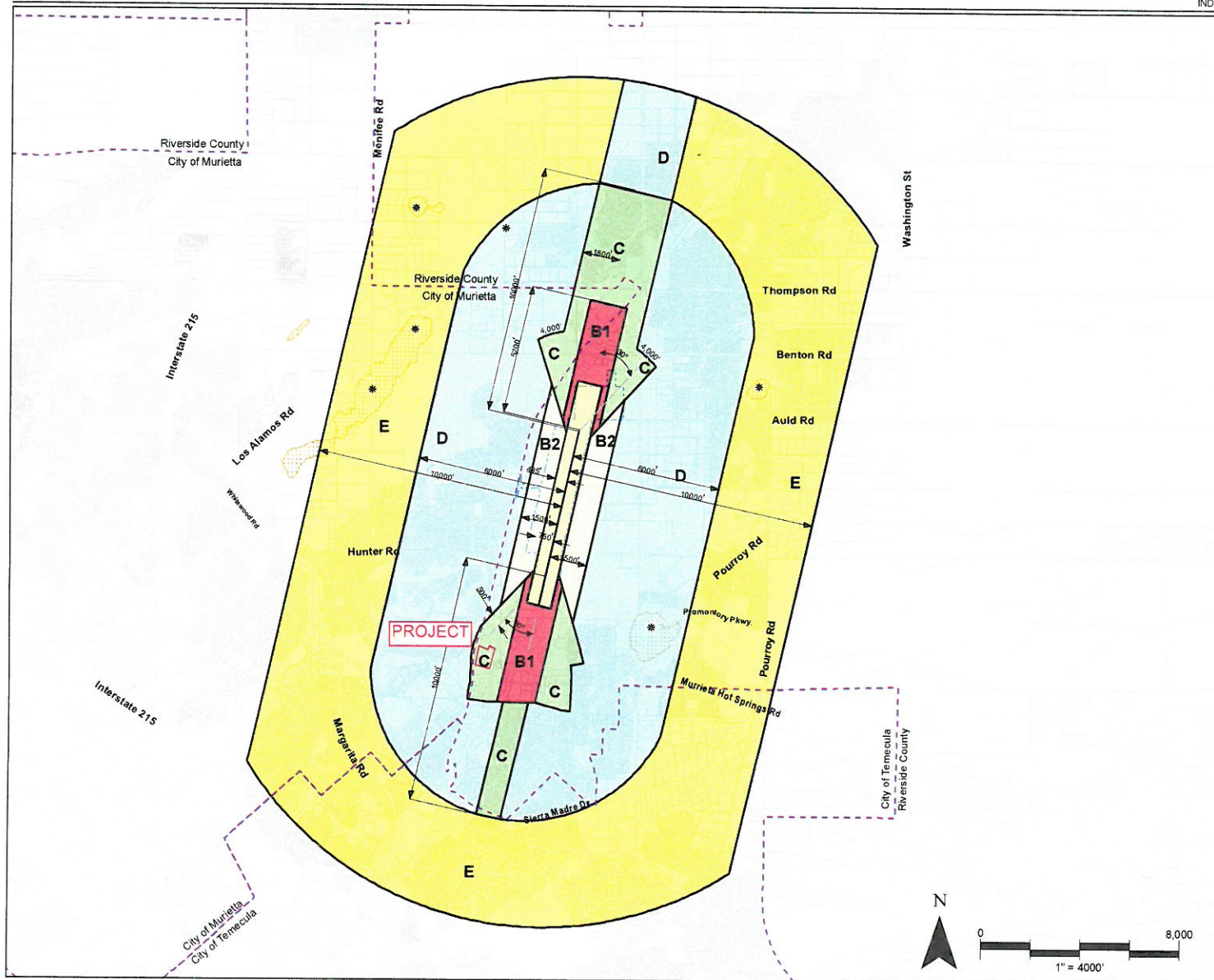
**PROPER MAINTENANCE IS NECESSARY TO AVOID  
BIRD STRIKES**



**IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:**

**Name:** \_\_\_\_\_

**Phone:** \_\_\_\_\_



**Legend**

**Compatibility Zones**

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E

**Boundary Lines**

- Airport Property Line
- City Limits
- \* Height Review Overlay Zone

**Note**

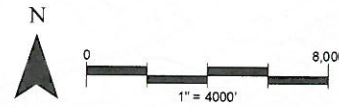
Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

See Chapter 2, Table 2A from compatibility criteria associated with this map.

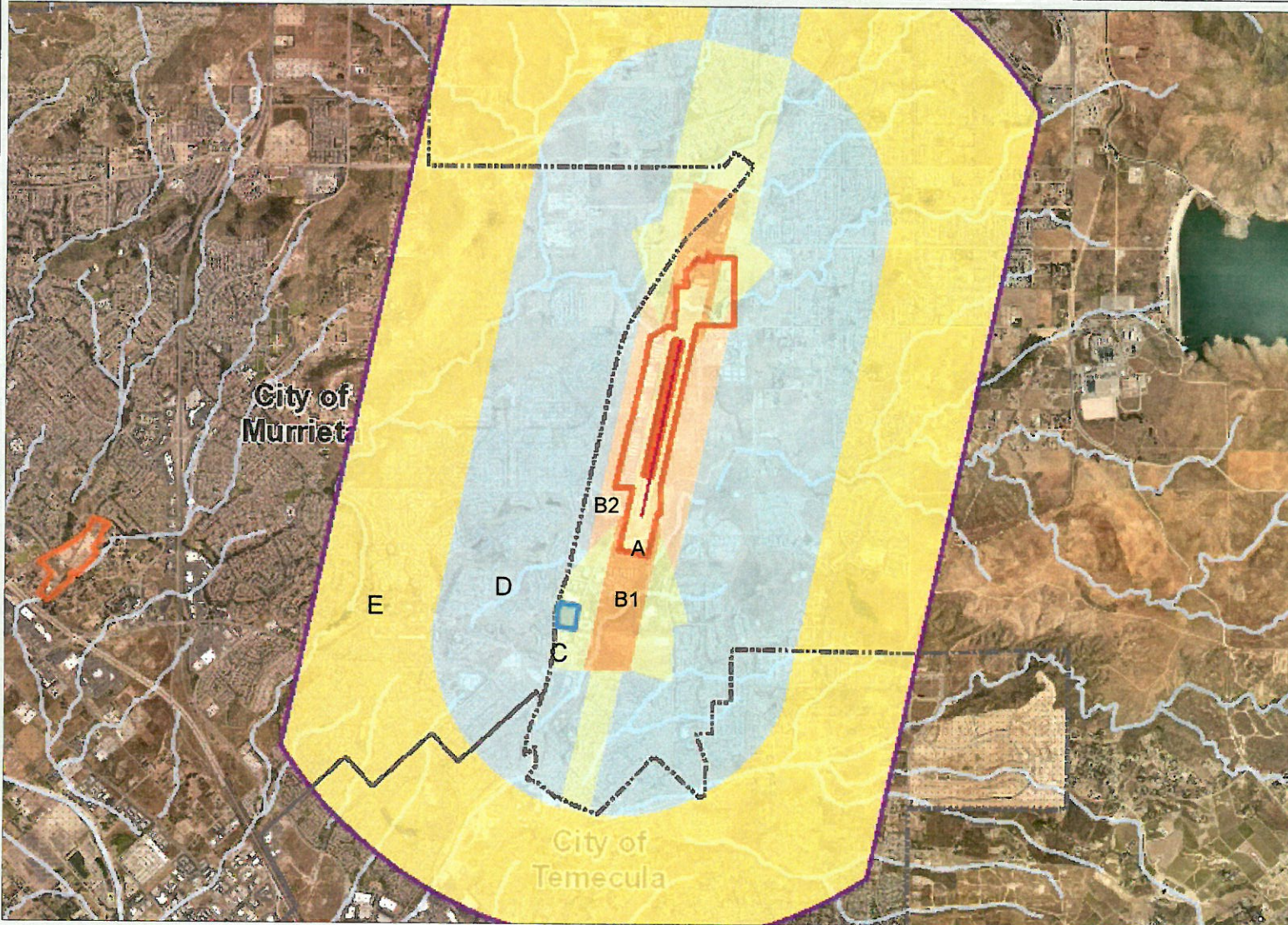
Riverside County  
 Airport Land Use Commission  
**Riverside County  
 Airport Land Use Compatibility Plan  
 Policy Document**  
 (April 2010)

Map FV-1

**Compatibility Map**  
 French Valley Airport



# Map My County Map



## Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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## Notes



# Map My County Map



## Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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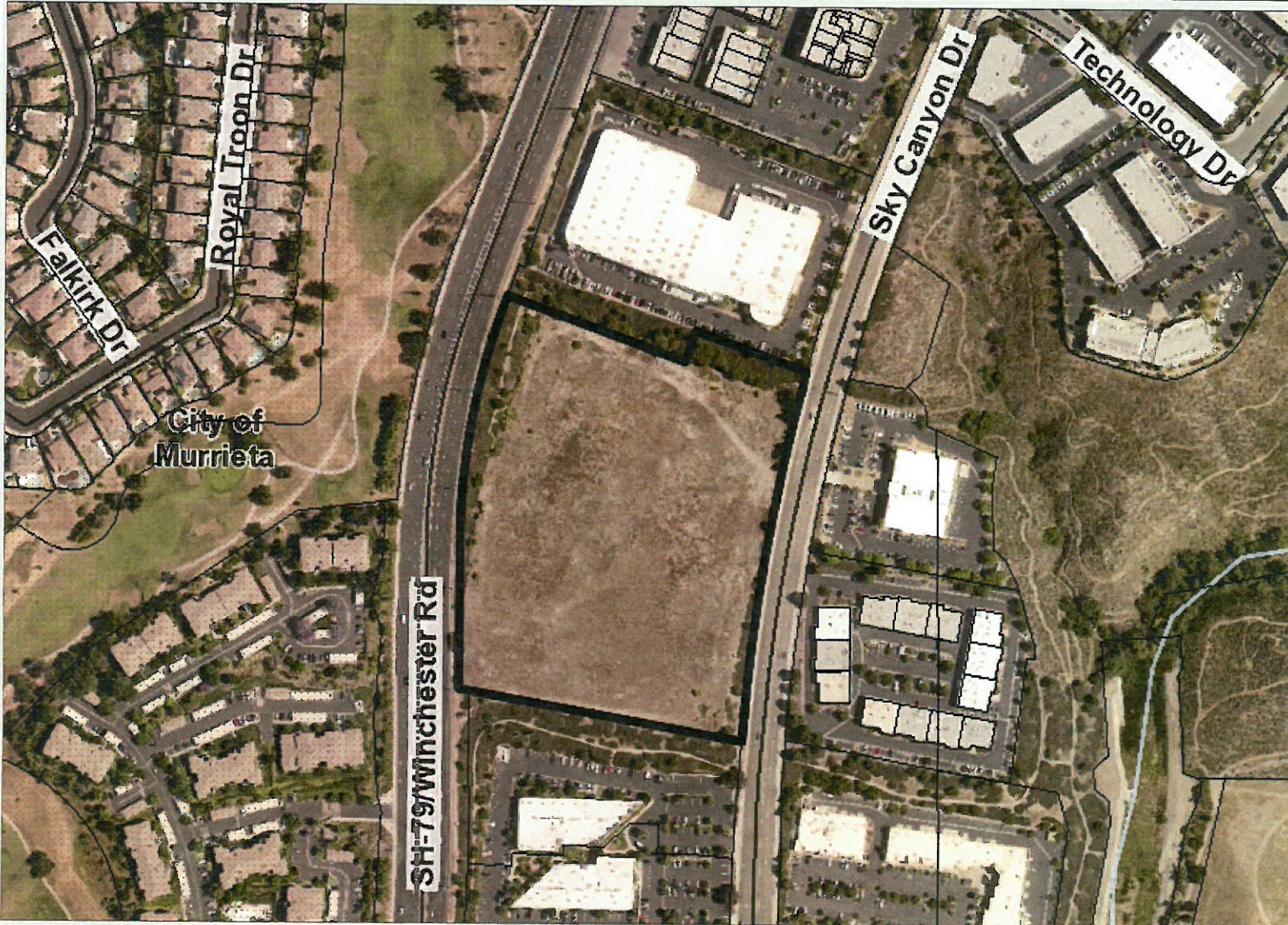
## Notes

0 385 770 Feet

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# Map My County Map



- Legend**
- Parcels
  - County Centerline Names
  - County Centerlines
  - Blueline Streams
  - City Areas
  - World Street Map



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**Notes**



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# Map My County Map



- Legend**
- Blueline Streams
  - ▣ City Areas
  - World Street Map



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**Notes**



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# Map My County Map



### Legend

- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



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### Notes



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# Map My County Map



### Legend

- County Centerline Names
- County Centerlines
- BlueLine Streams
- City Areas
- World Street Map



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### Notes



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# PROPOSED SHELL BUILDINGS FOR:

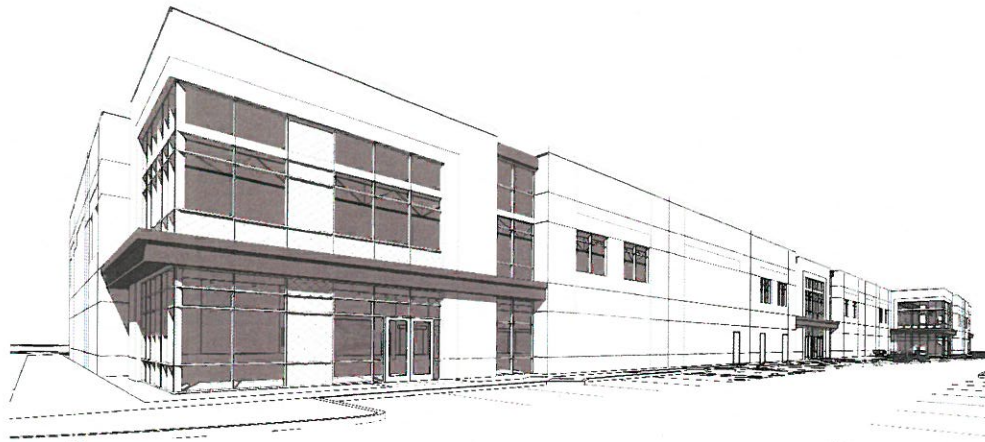


## DIAMOND HAWK DISTRIBUTION CENTER



1000 PIONEER WAY  
EL CAJON, CA 92020

DIAMOND HAWK  
DISTRIBUTION  
CENTER



### PROJECT INFORMATION

ASSESSOR'S PARCEL NUMBER	007332424
LEGAL	PORTION PARCEL 4 PM 16813-18 PARCEL MAP NO. 32348-1 REC 1/21/1989
EXISTING STRUCTURES ON SITE	NONE
EXISTING USE	RAW LAND
PROPOSED USE	OFFICE & WAREHOUSE
EXISTING ZONE	LP
BUILDING ADDRESS	T.B.O. RIVERSIDE COUNTY, CA
REQUIRED PERMITS	BUILDING PERMIT AD PROPOSED
BUILDING HEIGHT	TYPE IIR - FULLY SPRINKLED
CONSTRUCTION TYPE	TYPE IIR - FULLY SPRINKLED
LAND USE INFORMATION	SPECIFIC PLAN 210 PLANNING AREA 2 COUNTY OF RIVERSIDE - FEMOULA SPHERE OF INFLUENCE UP ZONE REGULATIONS (PER ARTICLE 8.000, 8.04.01(1))
DEVELOPMENT STANDARDS	
MAX HEIGHT	35' AT SETBACK LINE INCREASING 1" IN HEIGHT FOR EVERY ADDITIONAL 2' ADDED TO THE SETBACK. (MAX 50')
SETBACKS	STREET 20' SIDE YARD 10' REAR YARD 15' WHEN PROP. ABUTS RESI. COMMERCIAL ZONED PROPERTY 50'
FLOOR AREA RATIO	432,228 S.F. @ 50% = MAX FAR 216,114 S.F.
ROOF SCREENS	ALL ROOF MOUNTED EQUIPMENT SHALL BE SCREENED FROM THE GROUND ELEVATION VIEW TO A MINIMUM SHORT DISTANCE OF 120'
BUILDING AREA ANALYSIS	50% SPRINKLERED ONE- STORY BUILDINGS
THE AREA OF A GROUP OF 8' M OR 5 BUILDINGS MORE THAN ONE STORY ABOVE GRADE PLANE OF ANY CONSTRUCTION TYPE SHALL NOT BE LIMITED WHERE THE BUILDING IS PROVIDED WITH AN AUTOMATIC SPRINKLER SYSTEM THROUGHOUT IN ACCORDANCE WITH SECTION 903.1.1 AND IS SURROUNDED AND ADJONED BY PUBLIC WAYS OR YARDS NOT LESS THAN 10 FEET (10 MIN IN WIDTH, MINIMUM SEPARATION & PROVIDED SIDE YARDS	
	75'
<b>SITE AREA:</b>	
GROSS SITE AREA	432,228 S.F. (11.30 AC)
NET PAD	429,292 S.F. (9.85 AC)
<b>PROPOSED BUILDING FLOOR AREAS:</b>	
<b>BUILDING A</b>	
1 <sup>st</sup> FLOOR GROSS BUILDING AREA (DAP LINE)	73,214 S.F.
MEZZANINE GROSS BUILDING AREA	8,814 S.F.
GROSS BUILDING AREA	82,028 S.F.
TRASH ENCLOSURE AREA	902 S.F.
<b>BUILDING B</b>	
1 <sup>st</sup> FLOOR GROSS BUILDING AREA (DAP LINE)	88,891 S.F.
MEZZANINE GROSS BUILDING AREA	8,884 S.F.
GROSS BUILDING AREA	97,775 S.F.
TRASH ENCLOSURE AREA	902 S.F.
<b>SITE TOTAL:</b>	
BUILDING AREA	179,803 S.F.
TRASH ENCLOSURES	1,804 S.F.
<b>PARKING ANALYSIS:</b>	
STANDARD	263 SPACES
ACCESSIBLE	8 SPACES
ACCESSIBLE EV	3 SPACES
EVC'S	18 SPACES
TOTAL	292 SPACES
PARKING RATIO	1.64 (1/100)

### SHEET INDEX

NO.	SHEET NAME
<b>GENERAL SHEETS</b>	
P1	PROJECT INFORMATION
<b>CIVIL</b>	
1	PRELIMINARY GRADING PLAN
<b>LANDSCAPE</b>	
L1	PRELIMINARY LANDSCAPE PLAN
<b>ARCHITECTURAL</b>	
P2	CONCEPT SITE PLAN
P3	BUILDING A FLOOR PLANS
P4	BUILDING B FLOOR PLANS
P5	BUILDING A ELEVATIONS
P6	BUILDING B ELEVATIONS
P7	IMAGES
P8	IMAGES

Date	Issued For
04.10.21	RIVERSIDE PLANNING REVIEW
07.10.21	CLIENT REVIEW
08.24.21	RIVERSIDE COUNTY PLANNING SUBMITTAL
02.14.22	RIVERSIDE COUNTY PLANNING RE-SUBMITTAL

### VICINITY MAP - NTS



### ADDITIONAL SITE INFO

- PROJECT IS LOCATED WITHIN THE FOLLOWING
1. HIGHWAY 79 POLICY AREA
  2. COUNTY SERVICE AREA #43
  3. FRENCH VALLEY AIRPORT INFLUENCE AREA, ZONE C
  4. MURKIN/HOT SPOTS/FULT COUNTY FAULT ZONE
  5. HEM PALEONTOLOGICAL SENSITIVITY
  6. SUSCEPTIBLE TO DROUGHT
  7. APPLICABLE UTILITY PROVIDERS
    - A. EASTERN MUNICIPAL WATER DISTRICT
    - B. CAL EDISON



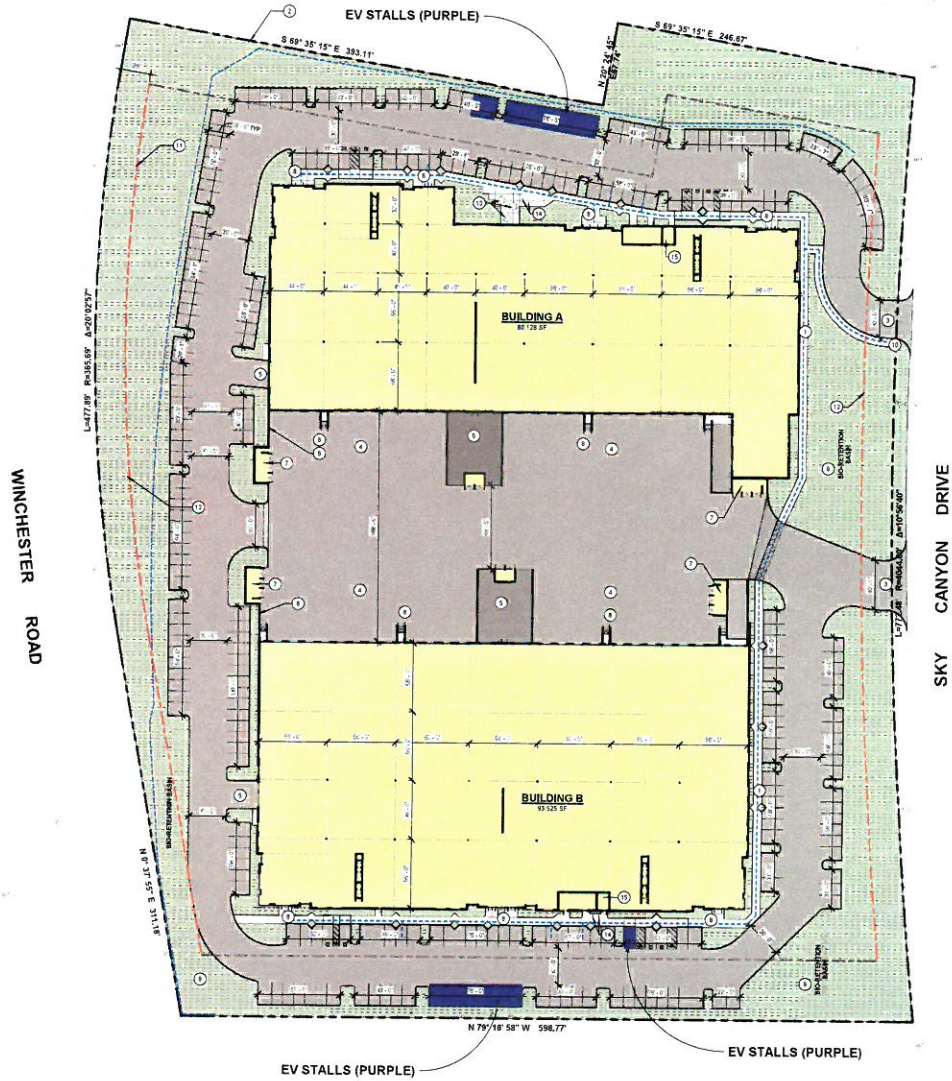
225 Broadway  
Suite 1100  
San Diego, California  
92101 USA  
619-284-0850  
WWW.HED.DESIGN

NOT FOR CONSTRUCTION

2/14/22  
PROJECT  
INFORMATION

PLOT PLAN NO. 210140

P1



OVERALL SITE PLAN KEYNOTES: ①

- 1 PEDESTRIAN ACCESS
- 2 PROPERTY LINE
- 3 PRIMARY ROAD ENTRY LOCATION
- 4 TRUCK LOADING/DOCK AREA
- 5 GRADE LEVEL DOOR
- 6 TRUCK SCREEN WALL
- 7 TRASH ENCLOSURE
- 8 BUILDING ENTRY
- 9 BIOWHALE
- 10 ACCESS TO PUBLIC WAY
- 11 50' SETBACK FROM COMMERCIAL PROPERTY
- 12 20' STREET SIDE SETBACK
- 13 PRECAST CONCRETE TABLE/BENCHES WHOLESALE FOR UMBRELLA
- 14 BIKE RACKS
- 15 SECURED BIKE LOCKERS

PROJECT SITE WILL NOT BE GATED



1000 PIONEER WAY  
EL CAJON, CA 92020

DIAMOND HAWK  
DISTRIBUTION  
CENTER

Date	Issued For
06.10.21	PAKESIDE PLANNING REVIEW
07.18.21	CLIENT REVIEW
08.24.21	PAKESIDE COUNTY PLANNING SUBMITTAL
02.14.22	PAKESIDE COUNTY PLANNING RE-SUBMITTAL



225 Broadway  
Suite 1500  
San Diego, California  
92101 USA  
(619) 398-3800  
WWW.HED.DESIGN

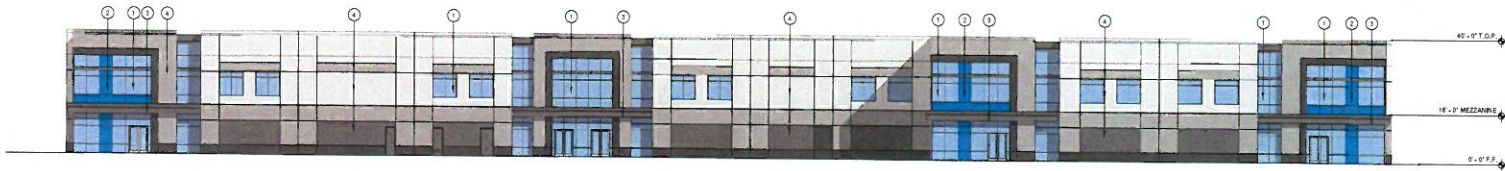
NOT FOR CONSTRUCTION

OVERALL SITE PLAN

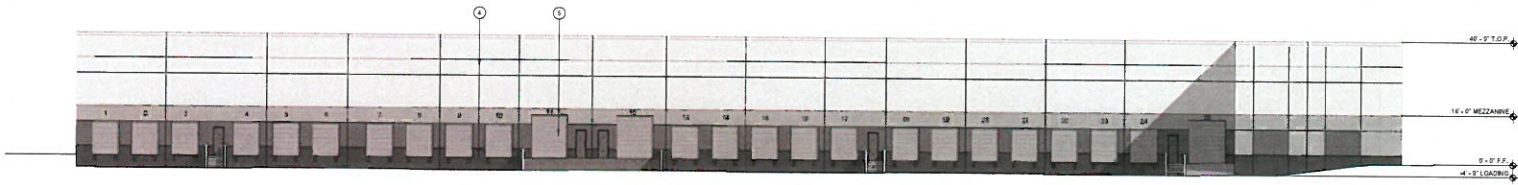
SCALE: 1" = 40'-0"

PLOT PLAN NO. 210140

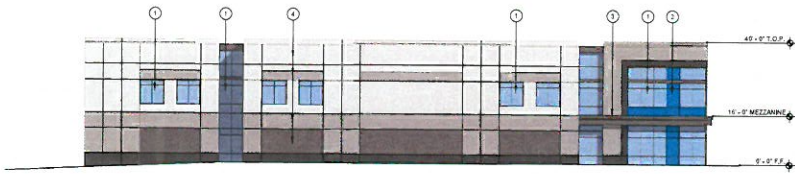
2/4/22  
CONCEPT SITE  
PLAN



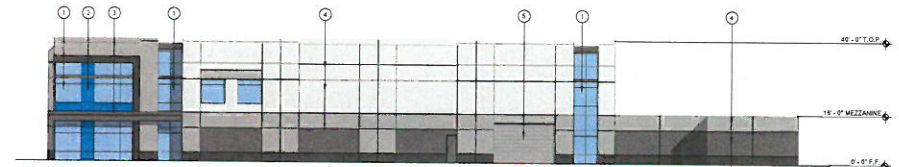
**NORTH ELEVATION**  
SCALE: 1/8" = 1'-0"



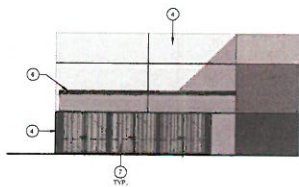
**SOUTH ELEVATION**  
SCALE: 1/8" = 1'-0"



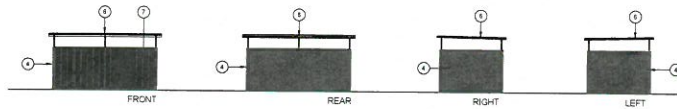
**EAST ELEVATION**  
SCALE: 1/8" = 1'-0"



**WEST ELEVATION**  
SCALE: 1/8" = 1'-0"



**LARGE TRASH ENCLOSURE ELEVATION**  
SCALE: 1/8" = 1'-0"



**SMALL TRASH ENCLOSURE ELEVATIONS**  
SCALE: 1/8" = 1'-0"

**FINISH**

	P1	PANT WHITE
	P2	PANT LIGHT GRAY
	P3	PANT MEDIUM GRAY
	P4	PANT DARK GRAY

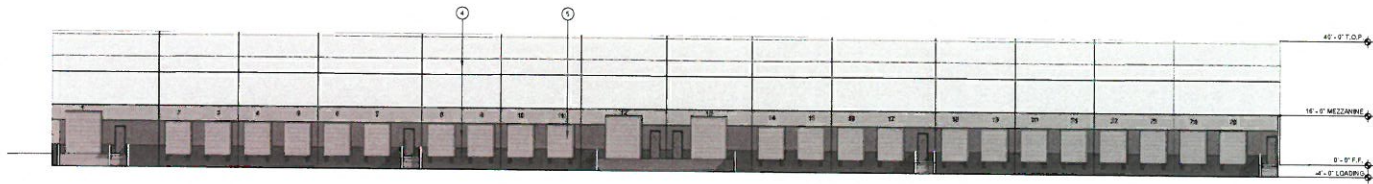
**EXTERIOR ELEVATION KEYNOTES: ①**

- 1 VISION GLASS
- 2 SPANDREL GLASS
- 3 ALUMINUM GLAZ METAL
- 4 PAINTED CONCRETE PANEL
- 5 ROLLUP DOOR TO RECEIVE SOUND REDUCING SEALS
- 6 METAL ROOF
- 7 METAL GATE

Date	Issued For
04.10.21	REVERSE PLANNING REVIEW
07.14.21	CLIENT REVIEW
08.24.21	PLANDER COUNTY PLANNING COMMISSION
02.14.22	REVERSE COUNTY PLANNING RE-SUBMITTAL



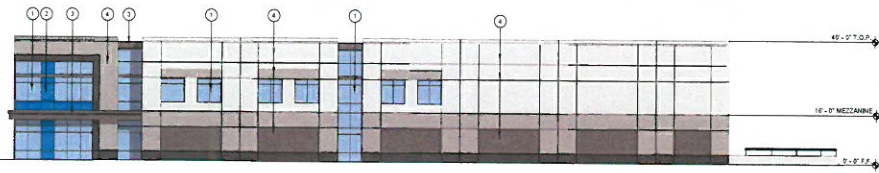
Date	Issued For
06.10.21	INVERLIDE PLANNING REVIEW
07.18.21	CLIENT REVIEW
08.24.21	INVERLIDE COUNTY PLANNING SUBMITTAL
02.14.22	INVERLIDE COUNTY PLANNING RE-SUBMITTAL



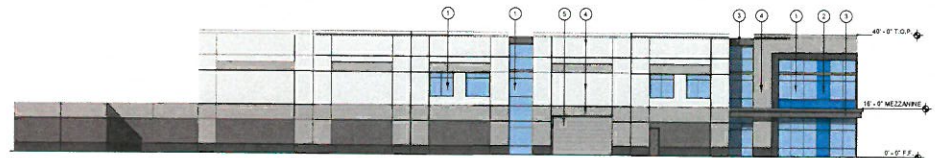
**NORTH ELEVATION**  
SCALE: 1/16" = 1'-0"



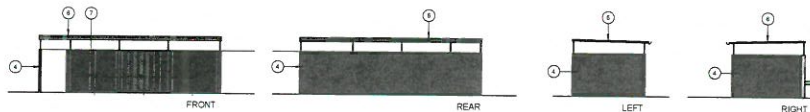
**SOUTH ELEVATION**  
SCALE: 1/16" = 1'-0"



**EAST ELEVATION**  
SCALE: 1/16" = 1'-0"



**WEST ELEVATION**  
SCALE: 1/16" = 1'-0"



**TRASH ENCLOSURE ELEVATIONS**  
SCALE: 1/8" = 1'-0"

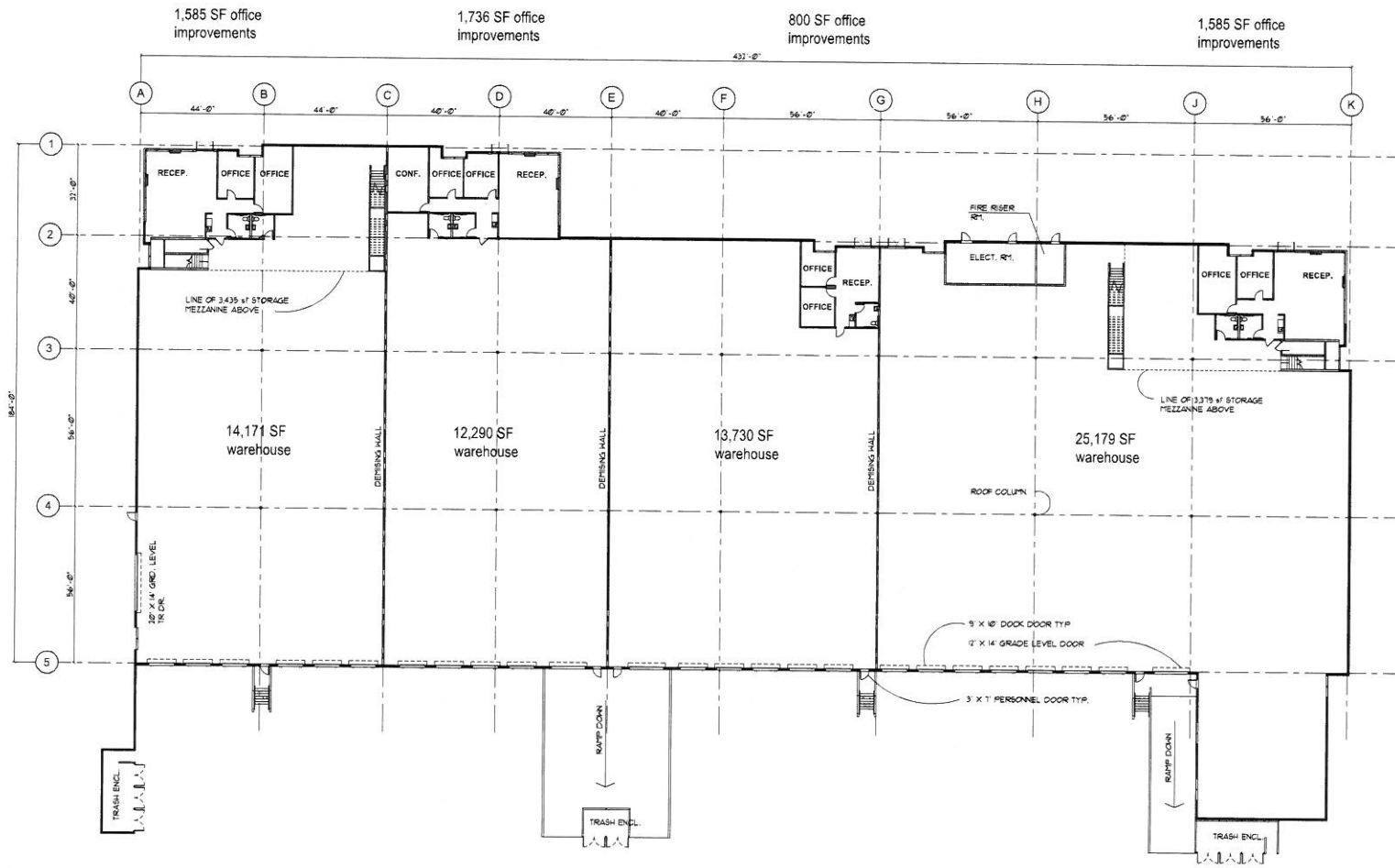
**FINISH**

	P1: PAINT WHITE
	P2: PAINT LIGHT GRAY
	P3: PAINT MEDIUM GRAY
	P4: PAINT DARK GRAY

**EXTERIOR ELEVATION KEYNOTES: ①**

- 1 VISION GLASS
- 2 SPANDREL GLASS
- 3 ALUMINUM CLAD METAL
- 4 PAINTED CONCRETE PANEL
- 5 ROLL-UP DOOR TO RECEIVE SOUND REDUCING SEALS
- 6 METAL ROOF
- 7 METAL GATE

**WEST ELEVATION**



80,128 SF to dripline

# FLOOR PLAN

SCALE: 1/8" = 1'-0"



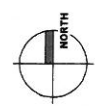
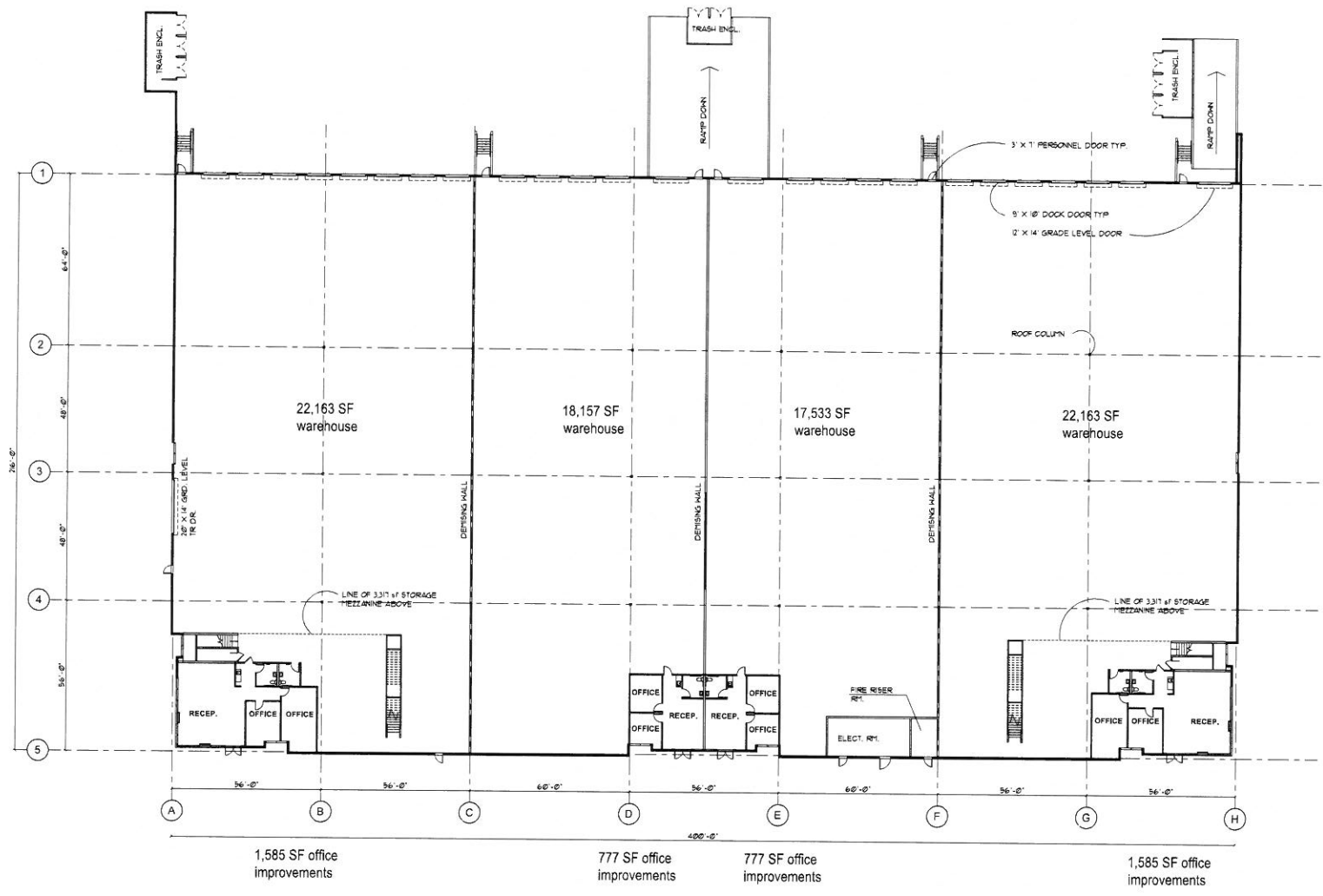
HAMANN CONSTRUCTION  
 1507 PIONEER WAY  
 EL CERRILLO, CA 95020  
 FAX 925-858-9214

DATE: \_\_\_\_\_  
 JOB NO: \_\_\_\_\_  
 DRAWN BY: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 REVISIONS:

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DIAMOND HAWK DISTRIBUTION  
 CENTER  
 SKY CANYON DRIVE  
 MURRIETTA, CALIFORNIA

A



93,525 SF to dripline

# FLOOR PLAN B

SCALE: 1/8" = 1'-0"



**HAMANN CONSTRUCTION**  
 1000 PIONEER WAY  
 EL CAJON, CA 92025  
 FAX 618-442-5714

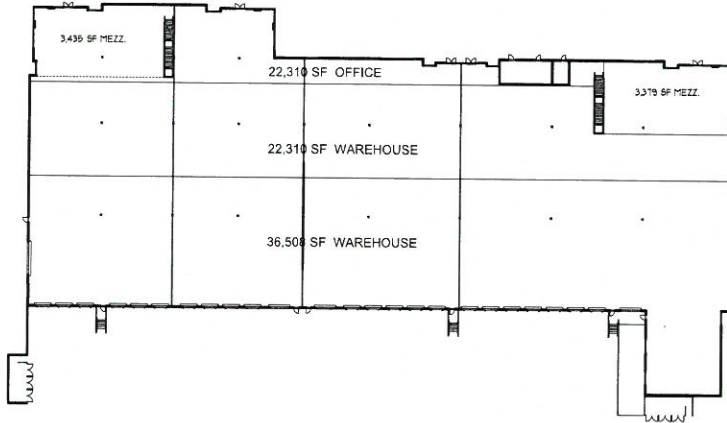
DATE: \_\_\_\_\_  
 JOB NO: \_\_\_\_\_  
 DRAWN BY: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 REVISIONS:

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**DIAMOND HAWK DISTRIBUTION CENTER**  
 SKY CANYON DRIVE  
 MURIELTA, CALIFORNIA

**B**

**BUILDING A**  
81,128 SF



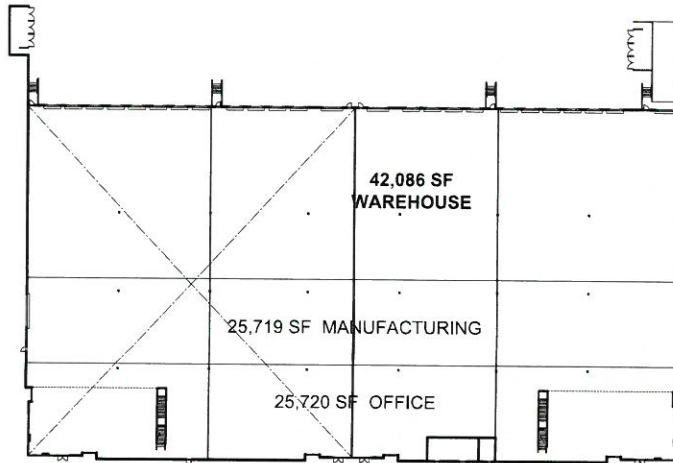
**DIAMOND HAWK - AIRPORT LAND USE COMMISSION CAPABILITY MATRIX**

BUILDING A	OFFICE	MANUFACTURING	WAREHOUSE	TOTAL
AREA	22,035	22,035	36,058	80,128 SQUARE FEET
OCCUPANT FACTOR	200 SF / OCC.	200 SF / OCC.	500 SF / OCC.	
	110	110	73	293 OCCUPANTS

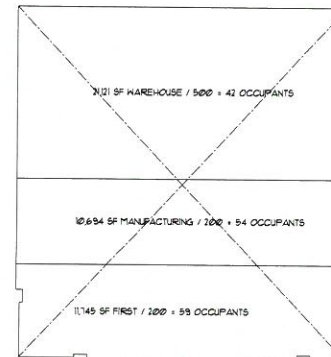
BUILDING B	OFFICE	MANUFACTURING	WAREHOUSE	TOTAL
AREA	25,720	25,719	42,086	93,525 SQUARE FEET
OCCUPANT FACTOR	200 SF / OCC.	200 SF / OCC.	500 SF / OCC.	
	129	129	84	342 OCCUPANTS

SITE TOTAL	OFFICE	MANUFACTURING	WAREHOUSE	TOTAL
AREA	47,755	47,754	78,144	173,653 SQUARE FEET
OCCUPANTS	239	239	157	635 OCCUPANTS

635 TOTAL SITE OCCUPANTS / 11.3 ACRES = 5 OCCUPANTS PER ACRE < 80 OCC. / ACRE ALLOWED



**BUILDING B**  
93,525 SF



TOTAL OCCUPANT LOAD 155 < 160 ALLOWED

**INTENSITY AREA CALCULATIONS**

**FLOOR PLANS**

SCALE: 1" = 30'-0"



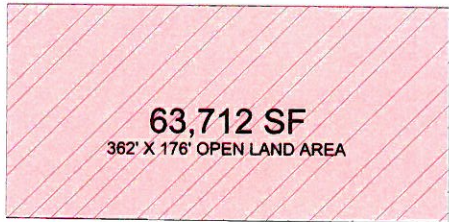
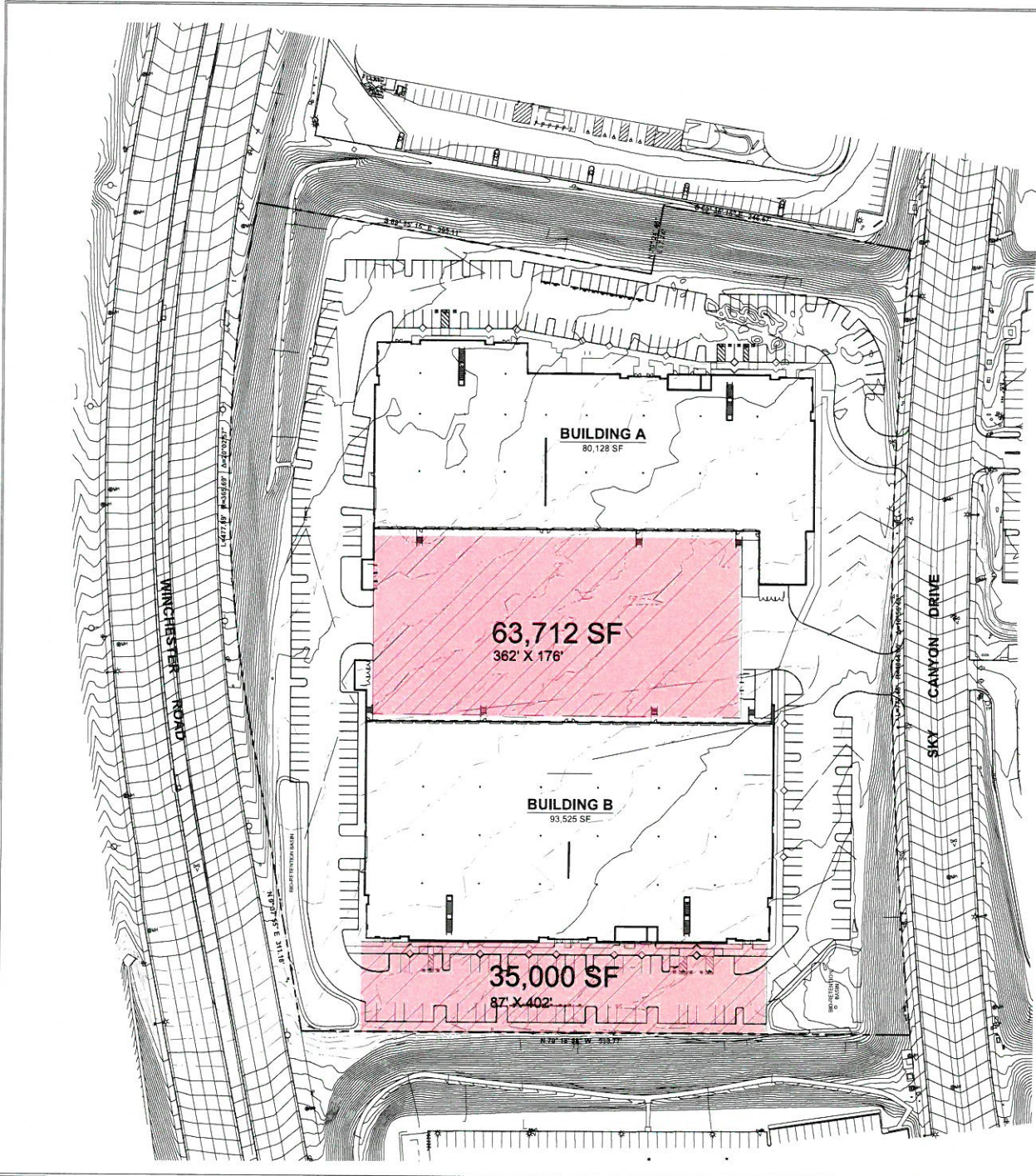
HAMANN CONSTRUCTION  
1047 PIONEER WAY  
EL CAJON, CA 92025  
TEL: 619.441.1111  
FAX: 619.441.1114

DATE: APRIL 1, 2012  
JOB NO:  
DRAWN BY:  
CHECKED BY:  
REVISIONS:

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DIAMOND HAWK  
CALIFORNIA  
TEMECULA

ALUC 2



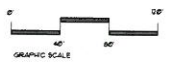
OPEN LAND AREA:  
SHALL HAVE A MINIMUM DIMENSION OF 75' X 300' AND BE FREE OF MOST STRUCTURE AND OTHER MAJOR OBSTACLES SUCH AS WALLS, LARGE TREES OR POLES (GREATER THAN 4" DIA. MEASURED 4 FEET ABOVE THE GROUND) AND OVERHEAD WIRES.

11.3 ACRES = 492,228 SF  
492,228 X 0.20 = 98,446 SF NEEDED  
98,712 SF PROVIDED



**SITE PLAN**

SCALE: 1" = 40'-0"



HAMANN CONSTRUCTION  
1000 PIONEER WAY  
EL CAJON, CA 92020  
TEL: 619-441-1111  
FAX: 619-442-9114

DATE: 11/11/14  
JOB NO: 14-001  
DRAWN BY: J. J.  
CHECKED BY: J. J.  
REVISIONS:

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DIAMOND HAWK  
TEMECULA CALIFORNIA

ALUC 1

**NOTICE OF PUBLIC HEARING**  
**RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**  
[www.rcaluc.org](http://www.rcaluc.org)

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. **Information on how to participate in the hearing will be available on the ALUC website at [www.rcaluc.org](http://www.rcaluc.org).** The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893.**

The County of Riverside Planning Department should be contacted on non-ALUC issues. For more information please contact County of Riverside Planner Deborah Bradford at (951) 922-6645.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website [www.rcaluc.org](http://www.rcaluc.org). Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to [prull@rivco.org](mailto:prull@rivco.org). Individuals with disabilities requiring reasonable modifications or accommodations, please contact Barbara Santos at (951) 955-5132.

**PLACE OF HEARING:**                 **Riverside County Administration Center  
4080 Lemon Street, 1<sup>st</sup> Floor Board Chambers  
Riverside California**

**DATE OF HEARING:**               **May 12, 2022**

**TIME OF HEARING:**               **9:30 A.M.**

**CASE DESCRIPTION:**

ZAP1117FV22 – HD Commercial Ventures, Inc. (Representative: Strom Entitlement Permitting PM, LLC) – County of Riverside Case No. PPT210140 (Plot Plan). A proposal to construct two industrial buildings totaling 173,653 square feet with mezzanines on 11.30 acres, located northerly of Murrieta Hot Springs Road, southerly of Technology Drive, westerly of Sky Canyon Drive, and easterly of Winchester Road. (Airport Compatibility Zone C of the French Valley Airport Influence Area).



# RIVERSIDE COUNTY

## AIRPORT LAND USE COMMISSION

### APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP1117FV22 DATE SUBMITTED: 3/3/22

**APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION**

Applicant	Gregg Hamann, CFO of HD Commercial Ventures, Inc., general partner of TOR Investments, LP	Phone Number	619.440.7424
Mailing Address	1000 Pioneer Way, El Cajon, CA 92020	Email	gregg@hamannco.com

Representative	Terry Strom - Strom Entitlement Permitting PM, LLC	Phone Number	951-970-7995
Mailing Address	300 Carlsbad Village Drive, STE 108A-74, Carlsbad, CA 92008	Email	terry@strompermit.com

Property Owner	Gregg Hamann, CFO of HD Commercial Ventures, Inc., general partner of TOR Investments, LP	Phone Number	619.440.7424
Mailing Address	Arnold Veldkamp, Manager of Brouwer Family LLC, general partner of JJB Silverhawk, L.P. 1000 Pioneer Way, El Cajon, CA 92020	Email	gregg@hamannco.com

**LOCAL JURISDICTION AGENCY**

Local Agency Name	County of Riverside	Phone Number	(951)955-6646
Staff Contact	Deborah Bradford, Planner	Email	dbradfor@rivco.org
Mailing Address	4080 Lemon Street, 12th Floor Riverside, CA 92501	Case Type	Plot Plan
Local Agency Project No	PPT210140	<input type="checkbox"/> General Plan / Specific Plan Amendment <input type="checkbox"/> Zoning Ordinance Amendment <input type="checkbox"/> Subdivision Parcel Map / Tentative Tract <input type="checkbox"/> Use Permit <input checked="" type="checkbox"/> Site Plan Review/Plot Plan <input type="checkbox"/> Other	

**PROJECT LOCATION**

*Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways*

Street Address	Vacant Land - No Street Address - The Project site is located north of Murrieta Hot Springs Road, south of Technology Drive, west of Sky Canyon Drive, and east of Winchester Road (Hwy. 79)		
Assessor's Parcel No.	957-330-054	Gross Parcel Size	11.30 Acres
Subdivision Name	N/A	Nearest Airport and distance from Airport	French Valley Airport - 3,600 ft
Lot Number	TR 23248-1, PM 168/12, Lot 4		

**PROJECT DESCRIPTION**

*If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed*

Existing Land Use (describe)	Vacant Land
------------------------------	-------------

Proposed Land Use (describe)	(2) single story warehouse / distribution buildings of 103,000 sf & 70,000 sf w/ associated parking and landscape. Buildings share a common 190' wide truck court x 375' in length. Gross land area: 11.3 acres Coverage of gross land area: 35% Total occupant load anticipated: 173 office + 130 warehouse = 303 occupants Density: 25.6 occupants / acre		
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units) _____		
For Other Land Uses (See Appendix C)	Hours of Operation _____		
	Number of People on Site	Maximum Number	_____
	Method of Calculation _____		
Height Data	Site Elevation (above mean sea level)	1,206	ft.
	Height of buildings or structures (from the ground)	40 Feet	ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?		<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
	If yes, describe _____ _____ _____		

- A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:**
1. . . . . Completed ALUC Application Form
  1. . . . . ALUC fee payment
  1. . . . . Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
  1. . . . . Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
  1. . . . . CD with digital files of the plans (pdf)
  1. . . . . Vicinity Map (8.5x11)
  1. . . . . Detailed project description
  1. . . . . Local jurisdiction project transmittal
  3. . . . . Gummed address labels for applicant/representative/property owner/local jurisdiction planner
  3. . . . . Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. **(Only required if the project is scheduled for a public hearing Commission meeting)**



# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

## STAFF REPORT

**AGENDA ITEM:** 3.3

**HEARING DATE:** May 12, 2022

**CASE NUMBER:** ZAP1509MA22 – Black & Veatch

**APPROVING JURISDICTION:** March Joint Powers Authority

**JURISDICTION CASE NO:** COM-Solar 22-003 (Building Permit)

**LAND USE PLAN:** 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

Airport Influence Area: March Air Reserve Base

Land Use Policy: Zone B2

Noise Levels: 65 - 70 CNEL from aircraft

**RECOMMENDATION:** Staff recommends that the Commission find the proposed Building Permit CONSISTENT with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, subject to the conditions included herein.

**PROJECT DESCRIPTION:** A proposal to construct an 84,000 square foot rooftop solar panel system on an existing industrial building on 45 acres.

**PROJECT LOCATION:** The site is located northerly of Van Buren Boulevard, westerly of Opportunity Way, and easterly of Meridian Parkway, approximately 3,078 feet westerly of the northerly end of Runway 14-32 at March Air Reserve Base.

### BACKGROUND:

Non-Residential Land Use Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zone B2, which limits average intensity to 100 people per acre and 250 people per single acre. The proposed rooftop solar panels will not generate any occupancy.

March Air Reserve Base/United States Air Force Input: Given that the project site is located in Zone B2 westerly of the northerly runway at March Air Reserve Base, the March Air Reserve Base staff was notified of the proposal to add rooftop solar panels, and sent a solar glare hazard analysis study for their review. On April 8, 2022, the Air Force provided comments concurring with the analysis and conclusions of the glare study, with the added note that their general concerns regarding cumulative impacts of solar projects are being addressed in the ongoing March Compatibility Use Study update.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zone B2 (children's schools, day care centers, hospitals, nursing homes, libraries, hotels/motels, places of assembly, buildings with 3 aboveground floors, critical community facilities, highly noise-sensitive outdoor nonresidential uses and hazards to flight).

Flight Hazard Issues: Structure height, electrical interference, and reflectivity/glare are among the issues that solar panels in the airport influence area must address. The project's 84,000 square foot photovoltaic (PV) panel structures would be located on the rooftop of the existing industrial building within Compatibility Zone B2.

#### *Glint and Glare/Reflectivity*

Based on the Federal Aviation Administration's Interim Policy for Review of Solar Energy System Projects on Federally Obligated Airports, no glare potential or low potential for temporary after-image ("green" level) are acceptable levels of glare on final approach (within 2 miles from end of runway) for solar facilities located on airport property. However, potential for temporary after-image ("yellow" level) and potential for permanent eye damage ("red" level) are not acceptable levels of glare on final approach. No glare is permitted at air traffic control towers.

The project proposes 84,000 square feet of solar panels on the existing building rooftop with a fixed tilt of 10 degrees with no rotation, and an orientation of 194 degrees. The applicant has submitted a glare analysis utilizing the web-based Forge Solar, a copy of which is attached hereto. The analysis was based on a 2 mile straight in approach (as per FAA Interim Policy standards) to runways 14 and 32, and also based on the traffic patterns as identified by March Air Reserve Base staff (Runway 12/30 General Aviation, Runway 14/32 General Aviation, Runway 14/32 C-17/KC-135, Runway 14/32 Overhead). The analysis utilized a glide slope approach of 3.0 degrees. No glare would affect the Air Traffic Control Tower.

The analysis concluded that some glare would occur on the 2 mile approach to the runways, and some potential for glare was identified within the Air Force traffic pattern. Evaluation of the Air Force traffic patterns indicates that the panels would result in a low potential for temporary after-image ("green" level glare) or no glare. All times are in standard time.

Runway 12/30 General Aviation traffic pattern (totaling 10,526 minutes of 'green' level glare):

- Runway 12 Downwind, totaling 1,022 minutes of "green" level glare, lasting up to 20 minutes a day, between March to May and September to October from 5:00 p.m. to 6:00 p.m.
- Runway 12 Upwind, totaling 2,761 minutes of "green" level glare, lasting up to 35 minutes a day, between May to September, from 6:00 p.m. to 7:00 p.m.
- Runway 12 Final, totaling 663 minutes of "green" level glare, lasting up to 15 minutes a day, in March to May and September to October, from 5:00 p.m. to 6:30 p.m.
- Runway 30 Upwind, totaling 663 minutes of "green" level glare, lasting up to 15 minutes a day, between March to May and September to October, from 5:00 p.m. to 6:00 p.m.
- Runway 30 Downwind, totaling 1,011 minutes of "green" level glare, lasting up to 20 minutes a day, in March to April and September to October from 5:00 p.m. to 6:00p.m.
- Runway 30 Final, totaling 2,765 minutes of "green" level glare, lasting up to 35 minutes a day, between May to September, from 6:00 p.m. to 7:00 p.m.

Runway 14/32 General Aviation traffic pattern (total 10,442 minutes of 'green' level glare):

- Runway 14 Downwind, totaling 4,399 minutes of "green" level glare, lasting up to 35 minutes a day, between September to April from 7:00 a.m. to 8:00 a.m.
- Runway 32 Downwind, totaling 4,402 minutes of "green" level glare, lasting up to 35 minutes a day, between September to April from 7:00 a.m. to 8:00 a.m.

Runway 14/32 C-17/KC-135 traffic pattern (totaling 6,855 minutes of 'green' level glare):

- Runway 14 Downwind, totaling 2,597 minutes of "green" level glare, lasting up to 25 minutes a day, between October to April from 6:00 a.m. to 7:30 a.m.
- Runway 32 Downwind, totaling 2,617 minutes of "green" level glare, lasting up to 25 minutes a day, between October to April from 6:00 a.m. to 7:30 a.m.

Runway 14/32 Overhead Aviation traffic pattern (totaling 10,223 minutes of 'green' level glare):

- Runway 14 Initial, totaling 5,903 minutes of "green" level glare, lasting up to 45 minutes a day, between April to September from 4:00 p.m. to 5:00 p.m.
- Runway 14 Downwind, totaling 2,679 minutes of "green" level glare, lasting up to 25 minutes a day, between October to April from 6:00 a.m. to 7:30 a.m.

The total of 38,046 minutes of "green" level glare represents less than 14 percent of total day light time.

#### *Electrical and Communication Interference*

The applicant has indicated that they do not plan to utilize equipment that would interfere with aircraft communications. The PV panels themselves present little risk of interfering with radar transmission due to their low profiles. In addition, solar panels do not emit electromagnetic waves over distances that could interfere with radar signal transmissions, and any electrical facilities that do carry concentrated current will be buried beneath the ground and away from any signal transmission. There are no radar transmission or receiving facilities within the site.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site within the 65 - 70 CNEL range from aircraft noise. The proposed solar panels are a non-noise sensitive use, therefore no mitigation measures are necessary.

Part 77: The elevation of Runway 14-32 at its northerly terminus is 1,535 feet above mean sea level (AMSL). At a distance of approximately 3,078 feet from the project to the nearest point on the runway, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,566 feet AMSL. The site's finished floor elevation is 1,564 feet AMSL and existing building height is 50 feet, resulting in a top point elevation of 1,614 feet AMSL. Therefore, review by the FAA Obstruction Evaluation Service was required. However, the height of the solar panels will not significantly increase the overall height of the building.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

#### **CONDITIONS:**

1. Any outdoor lighting installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site.
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight or circling climb following takeoff or toward an aircraft engaged in a straight or circling final approach toward a landing at an airport, other than a DoD or FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport to the extent as to result in a potential for temporary after-image greater than the low ("green") level.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, congregate care facilities, hotels/motels, places of assembly (including, but not limited to places of worship and theaters), buildings with more than 2 aboveground habitable floors, hazardous materials and critical community infrastructure facilities.
  - (f) Highly noise-sensitive outdoor nonresidential uses. Examples of noise-sensitive outdoor nonresidential uses that are prohibited include, but are not limited to, major spectator-oriented sports stadiums, amphitheaters, concert halls and drive-in theaters.
  - (g) Hazards to Flight.
3. Prior to issuance of building permits, the landowner shall convey an avigation easement to the March Inland Port Airport Authority or its successor in interest, or provide evidence that such easement has been previously conveyed. The Airport Authority may waive this requirement in the event that the Authority determines that pre-existing avigation easements dedicated to the United States of America are sufficient to address its needs. Contact the March Joint Powers Authority at (951) 656-7000 for additional information.

4. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
5. The maximum height of the building, including all roof-mounted equipment including solar panels, if any, shall be limited to 50 feet, and the maximum top point elevation shall not exceed 1,614 feet above mean sea level unless a "Determination of No Hazard to Air Navigation" letter authorizing a higher top point elevation has been issued by the Federal Aviation Administration Obstruction Evaluation Service.
6. All solar arrays installed on the project site shall consist of smooth glass photovoltaic solar panels without anti-reflective coating, a fixed tilt of 10 degrees and orientation of 194 degrees. Solar panels shall be limited to a total of 84,000 square feet, and the locations and coordinates shall be as specified in the glare study. Any deviation from these specifications (other than reduction in square footage of panels), including change in orientation, shall require a new solar glare analysis to ensure that the amended project does not result in any glare impacting the air traffic control tower or creation of any "yellow" or "red" level glare in the flight paths, and shall require a new hearing by the Airport Land Use Commission.
7. In the event that any glint, glare, or flash affecting the safety of air navigation occurs as a result of project operation, upon notification to the airport operator of an event, the airport operator shall notify the project operator in writing. Within 30 days of written notice, the project operator shall be required to promptly take all measures necessary to eliminate such glint, glare, or flash. An "event" includes any situation that results in an accident, incident, "near-miss," or specific safety complaint regarding an in-flight experience to the airport operator or to federal, state, or county authorities responsible for the safety of air navigation. The project operator shall work with the airport operator to prevent recurrence of the incidence. Suggested measures may include, but are not limited to, changing the orientation and/or tilt of the source, covering the source at the time of day when events of glare occur, or wholly removing the source to diminish or eliminate the source of the glint, glare, or flash. For each such event made known to the project operator, the necessary remediation shall only be considered to have been fulfilled when the airport operator states in writing that the situation has been remediated to the airport operator's satisfaction.
8. In the event that any electrical interference affecting the safety of air navigation occurs as a result of project operation, upon notification to the airport operator of an event, the airport operator shall notify the project operator in writing. Within 30 days of written notice, the project operator shall be required to promptly take all measures necessary to eliminate such interference. An "event" includes any situation that results in an accident, incident, "near-miss," report by airport personnel, or specific safety complaint to the airport operator or to federal, state, or county authorities responsible for the safety of air navigation. The project operator shall work with the airport operator to prevent recurrence of the event. For each such event made known to the project operator, the necessary remediation shall only be considered to have been fulfilled when the airport operator states in writing that the situation has been remediated to the airport operator's satisfaction.

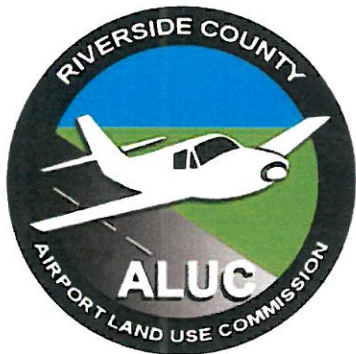
# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

# NOTICE

**THERE IS AN AIRPORT NEARBY.  
THIS STORM WATER BASIN IS DESIGNED TO HOLD  
STORM WATER FOR ONLY 48 HOURS AND  
NOT TO ATTRACT BIRDS**

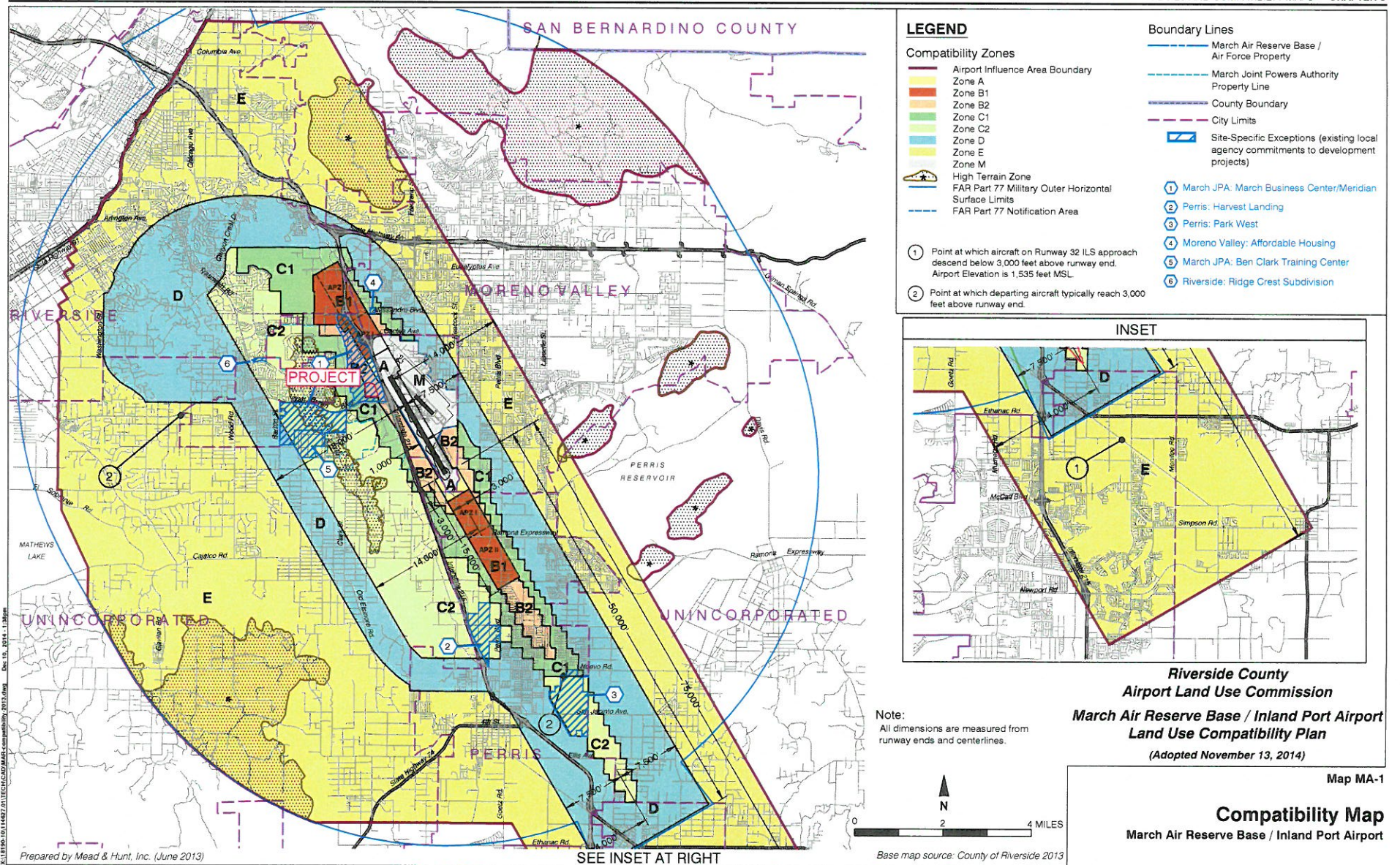
**PROPER MAINTENANCE IS NECESSARY TO AVOID  
BIRD STRIKES**



**IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:**

**Name:** \_\_\_\_\_

**Phone:** \_\_\_\_\_



**LEGEND**

**Compatibility Zones**

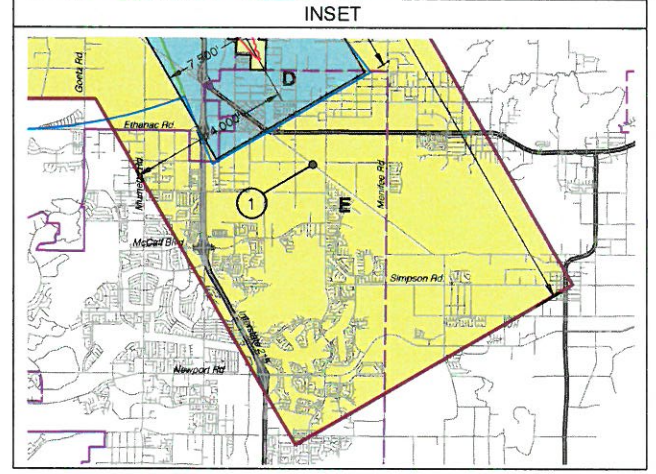
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

**Boundary Lines**

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

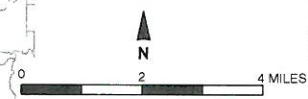
- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



**Riverside County  
Airport Land Use Commission  
March Air Reserve Base / Inland Port Airport  
Land Use Compatibility Plan  
(Adopted November 13, 2014)**

Note:  
All dimensions are measured from  
runway ends and centerlines.



Base map source: County of Riverside 2013

X:\11896\_10\114877\01\TECH\CD\MAPS\compatibility\_2013.dwg Dec 16, 2014 1:15pm

Prepared by Mead & Hunt, Inc. (June 2013)

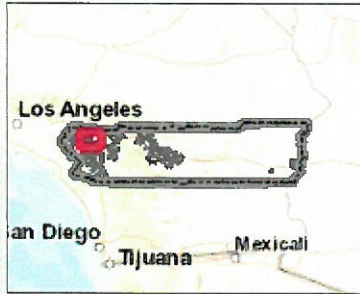
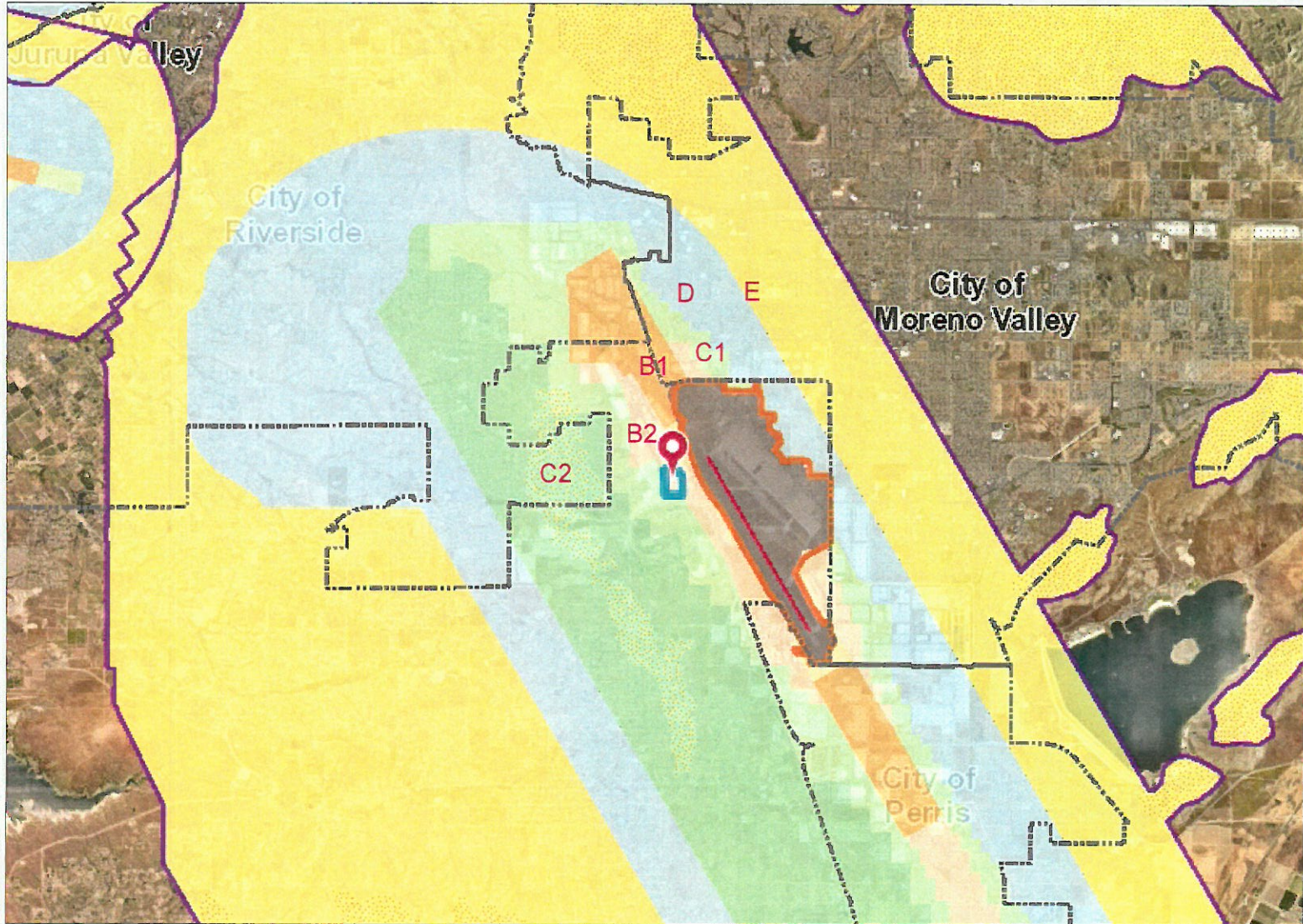
SEE INSET AT RIGHT

Map MA-1

**Compatibility Map  
March Air Reserve Base / Inland Port Airport**



# Map My County Map



### Legend

- Runways
- Airports
- Airport Influence Areas

#### Airport Compatibility Zones

- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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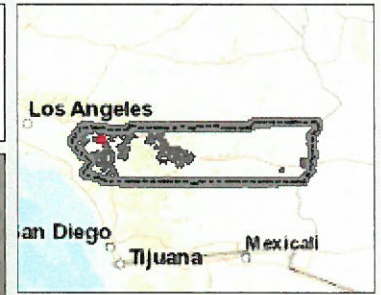
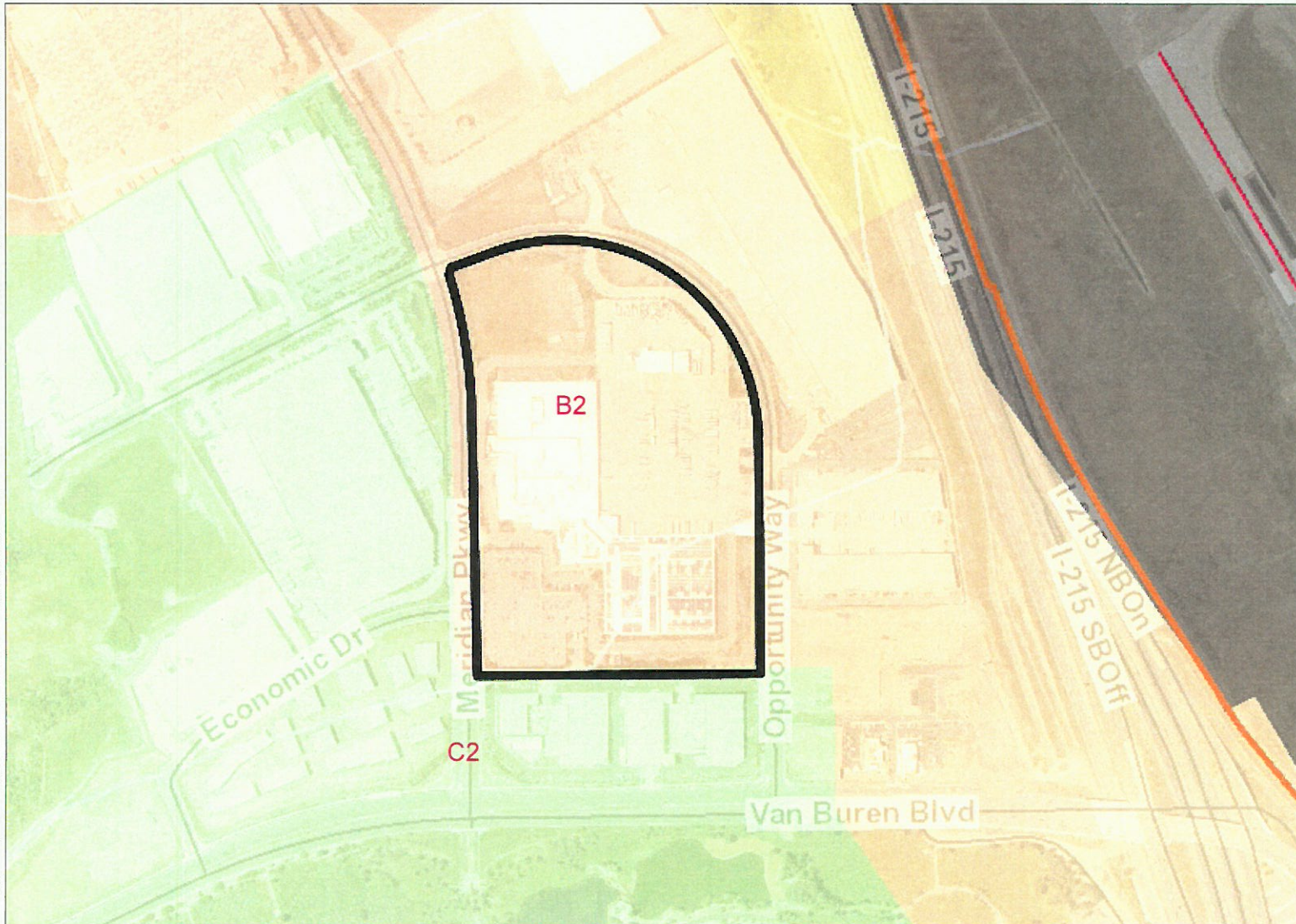
### Notes



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# Map My County Map



## Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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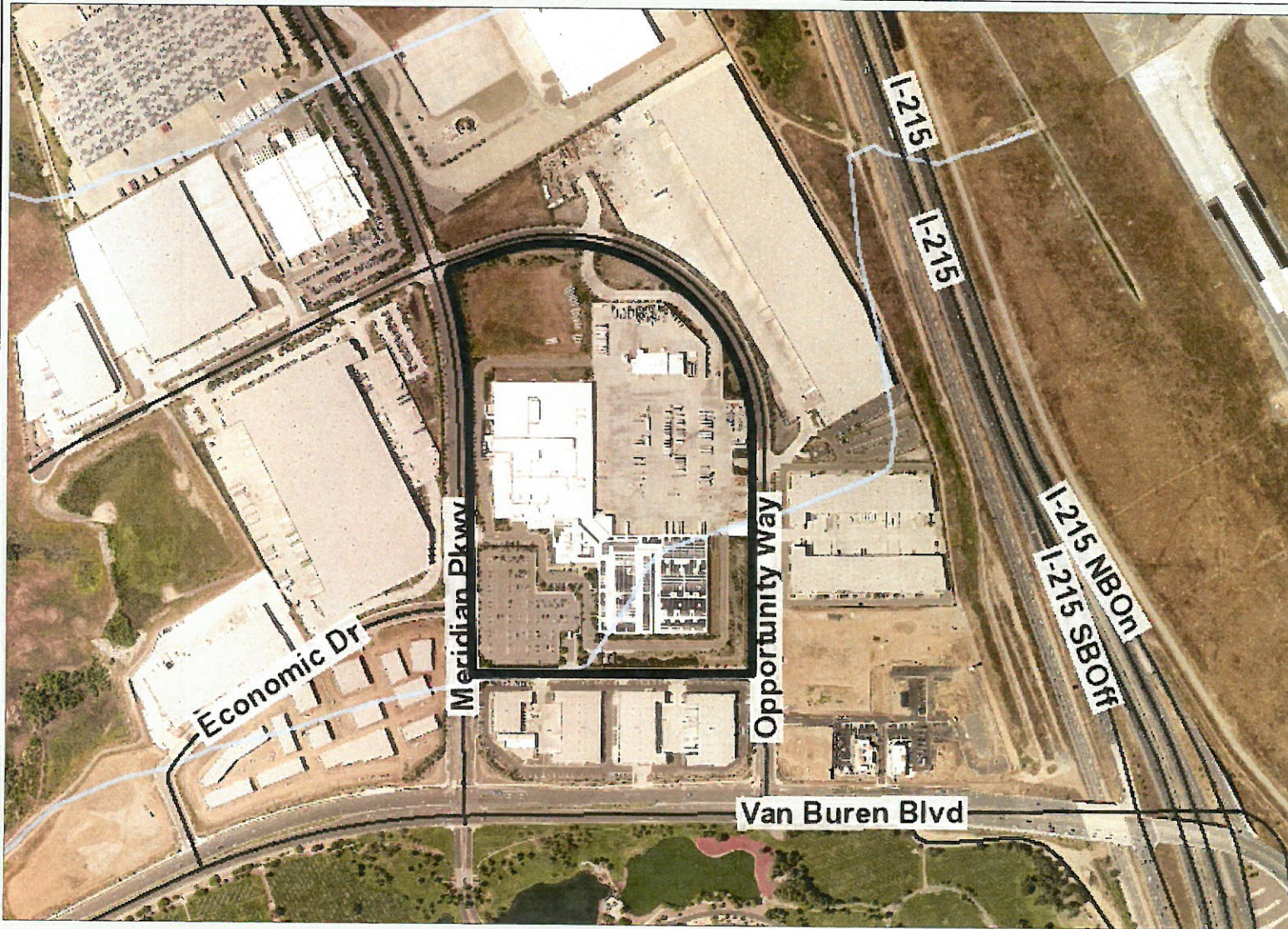
## Notes

0 770 1,539 Feet

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# Map My County Map



- Legend**
- County Centerline Names
  - County Centerlines
  - Blueline Streams
  - City Areas
  - World Street Map



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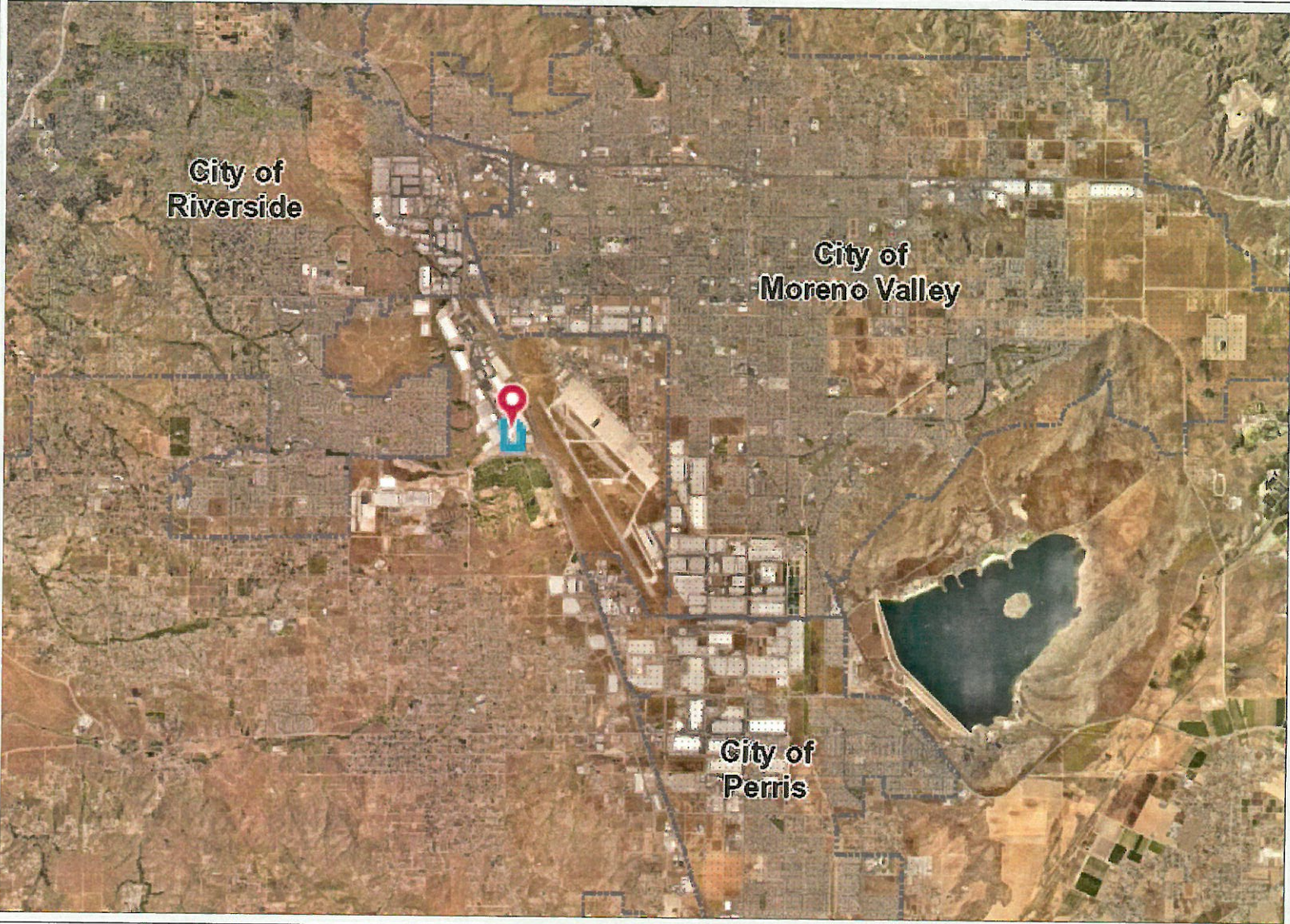
**Notes**



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# Map My County Map



- Legend**
- City Areas
  - World Street Map



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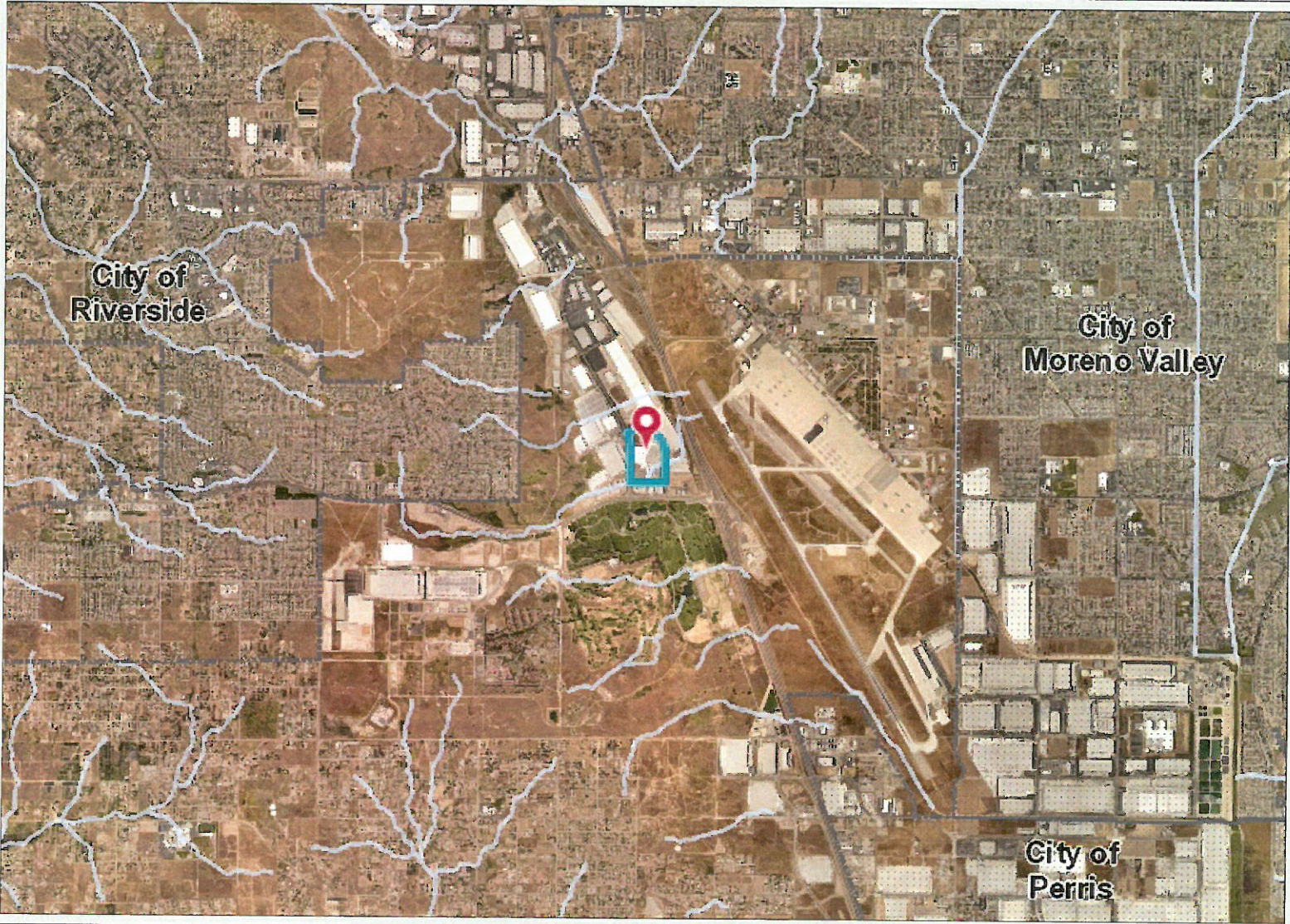
Notes



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# Map My County Map



- Legend**
- Blueline Streams
  - City Areas
  - World Street Map



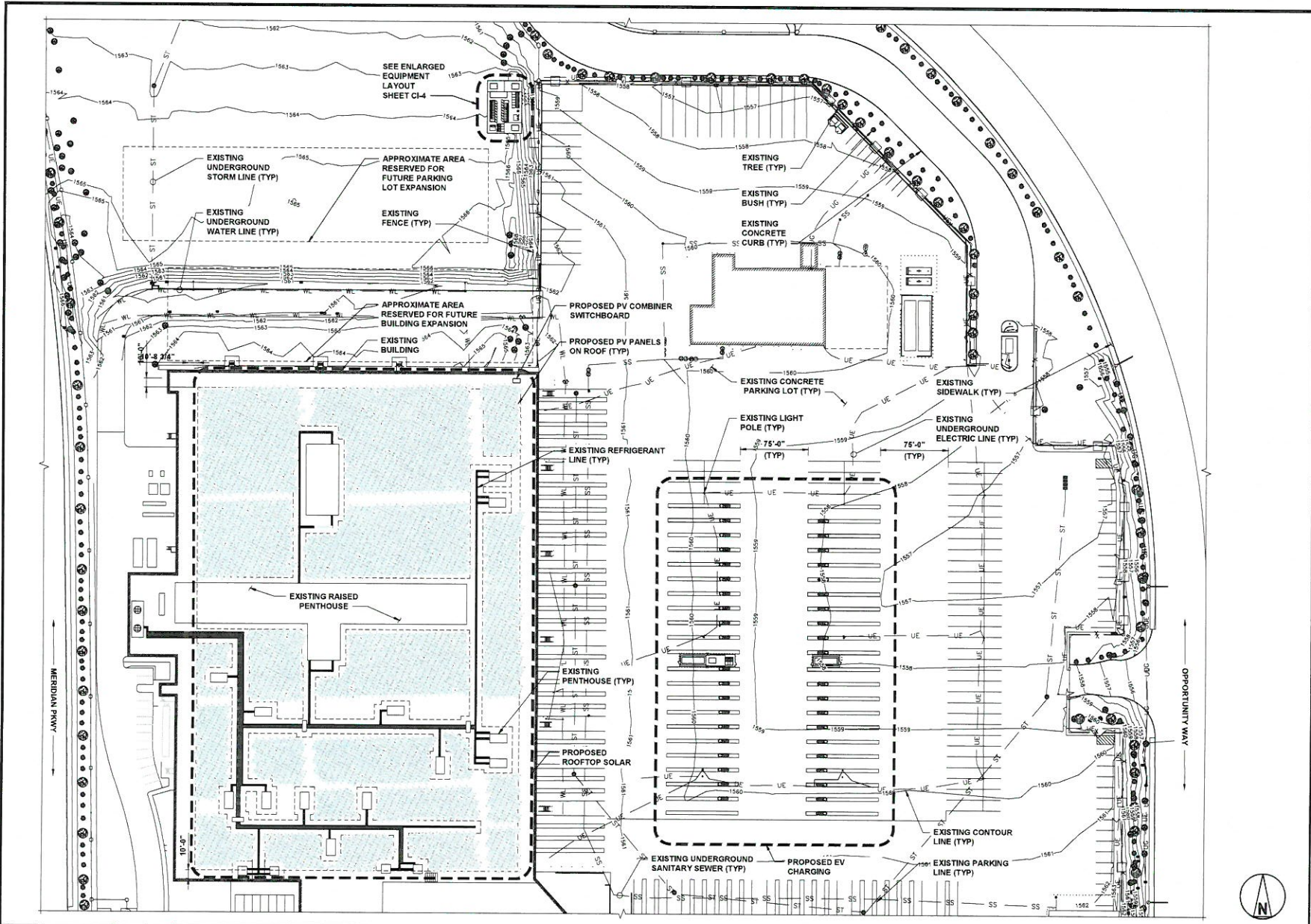
**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

**Notes**

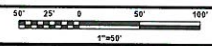


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OVERALL SITE PLAN



SYSCO RIVERSIDE  
15750 MERIDIAN PKWY  
RIVERSIDE, CA 92518



**BLACK & VEATCH**

6800 W 115th ST, SUITE 2282  
OVERLAND PARK, KS 66211  
(913) 458-2000

PROJECT NO:	409322
DRAWN BY:	VRS
CHECKED BY:	JDN

REV	DATE	DESCRIPTION
0	8/20/22	ISSUED FOR INTERCONNECTION

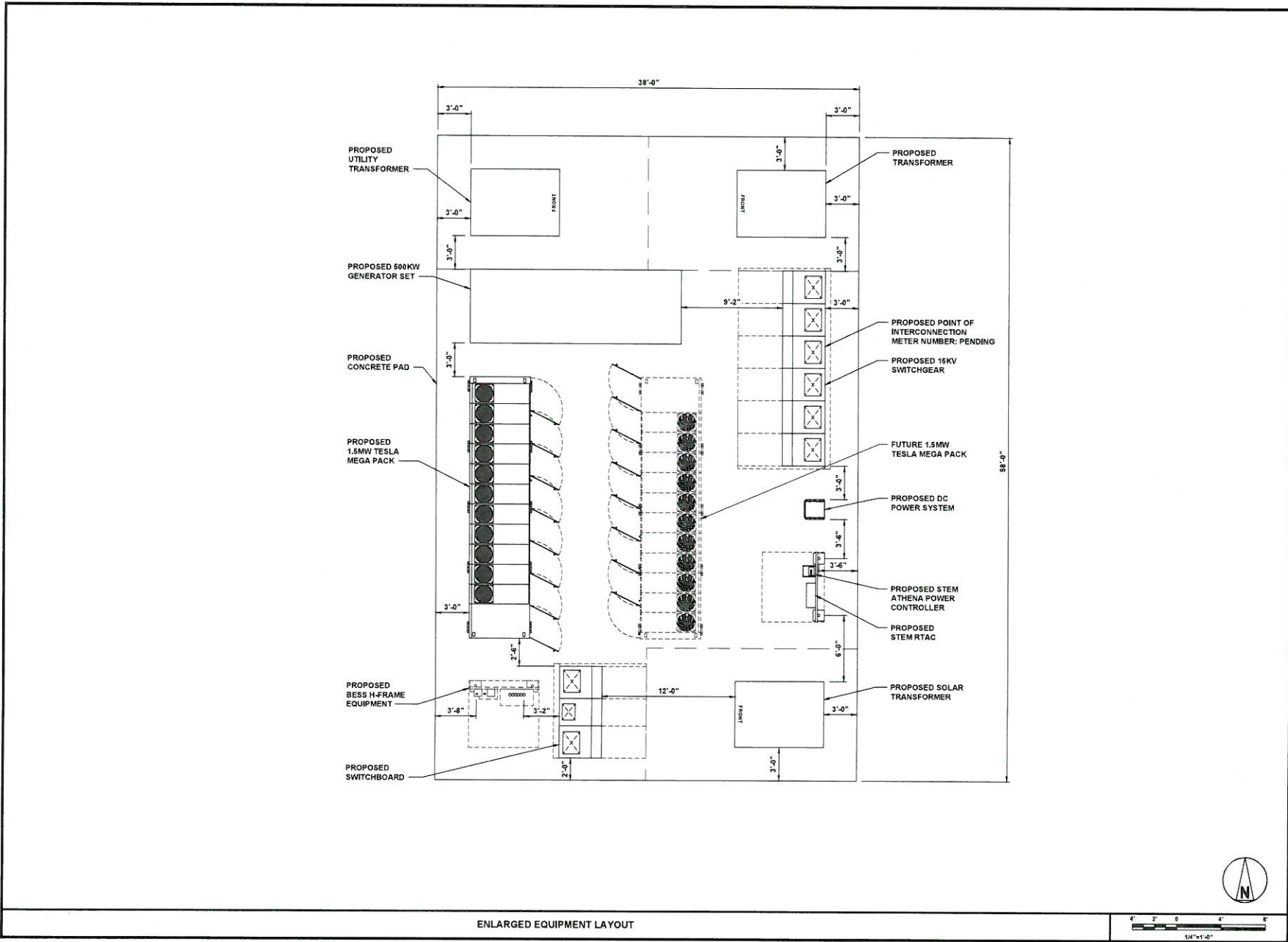
**NOT TO BE USED  
FOR CONSTRUCTION**

IT IS A VIOLATION OF LAW FOR ANY PERSON UNLESS  
THEY ARE ACTING UNDER THE DIRECTION OF A  
LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS  
DOCUMENT.

CA-SY001  
SYSCO RIVERSIDE  
15750 MERIDIAN PKWY  
RIVERSIDE, CA 92518

SHEET TITLE  
**OVERALL SITE PLAN  
INTERCONNECTION**

SHEET NUMBER  
**CI-1**



ENLARGED EQUIPMENT LAYOUT



SYSCO RIVERSIDE  
15750 MERIDIAN PKWY  
RIVERSIDE, CA 92518



**BLACK & VEATCH**

6800 W 115th ST, SUITE 2202  
OVERLAND PARK, KS 66211  
(913) 458-2000

PROJECT NO:	499322
DRAWN BY:	VRS
CHECKED BY:	JDN

REV	DATE	DESCRIPTION
0	02/01/22	ISSUED FOR INTERCONNECTION

**NOT TO BE USED FOR CONSTRUCTION**

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CA-SY001  
SYSCO RIVERSIDE  
15750 MERIDIAN PKWY  
RIVERSIDE, CA 92518

SHEET TITLE  
**ENLARGED EQUIPMENT LAYOUT INTERCONNECTION**

SHEET NUMBER  
**CI-2**



# FORGESOLAR GLARE ANALYSIS

Project: **Riverside CA 2**

Site configuration: **Site-2 5 Deg Tilt 205 Az**

Analysis conducted by Adam Kankiewicz (KankiewiczJA@bv.com) at 08:04 on 06 Jan, 2022.

## U.S. FAA 2013 Policy Adherence

The following table summarizes the policy adherence of the glare analysis based on the 2013 U.S. Federal Aviation Administration Interim Policy 78 FR 63276. This policy requires the following criteria be met for solar energy systems on airport property:

- No "yellow" glare (potential for after-image) for any flight path from threshold to 2 miles
- No glare of any kind for Air Traffic Control Tower(s) ("ATCT") at cab height.
- Default analysis and observer characteristics (see list below)

ForgeSolar does not represent or speak officially for the FAA and cannot approve or deny projects. Results are informational only.

COMPONENT	STATUS	DESCRIPTION
Analysis parameters	PASS	Analysis time interval and eye characteristics used are acceptable
2-mile flight path(s)	PASS	Flight path receptor(s) do not receive yellow glare
ATCT(s)	PASS	Receptor(s) marked as ATCT do not receive glare

Default glare analysis parameters and observer eye characteristics (for reference only):

- Analysis time interval: 1 minute
- Ocular transmission coefficient: 0.5
- Pupil diameter: 0.002 meters
- Eye focal length: 0.017 meters
- Sun subtended angle: 9.3 milliradians

FAA Policy 78 FR 63276 can be read at <https://www.federalregister.gov/d/2013-24729>





# Riverside CA 2

## Rwy 12 30 GA Rectangular Analysis

Created March 2, 2022  
 Updated April 12, 2022  
 Time-step 1 minute  
 Timezone offset UTC-8  
 Site ID 65635.11191

Project type Basic  
 Project status: active  
 Category 500 kW to 1 MW  
 (1,000 kW / 8 acre limit)



### Misc. Analysis Settings

DNI: varies (1,000.0 W/m<sup>2</sup> peak)  
 Ocular transmission coefficient: 0.5  
 Pupil diameter: 0.002 m  
 Eye focal length: 0.017 m  
 Sun subtended angle: 9.3 mrad

### Analysis Methodologies:

- Observation point: **Version 2**
- 2-Mile Flight Path: **Version 2**
- Route: **Version 2**

## Summary of Results Glare with low potential for temporary after-image predicted

PV Name	Tilt	Orientation	"Green" Glare	"Yellow" Glare	Energy Produced
	deg	deg	min	min	kWh
PV array 1	10.0	194.0	10,526	0	-

# Component Data

## PV Array(s)

Total PV footprint area: 5.2 acres

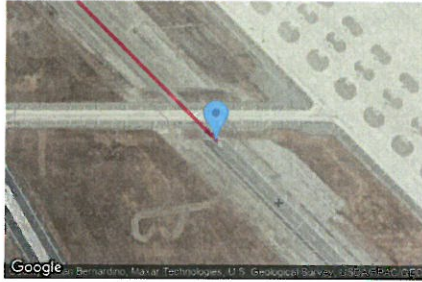
**Name:** PV array 1  
**Footprint area:** 5.2 acres  
**Axis tracking:** Fixed (no rotation)  
**Tilt:** 10.0 deg  
**Orientation:** 194.0 deg  
**Rated power:** -  
**Panel material:** Smooth glass without AR coating  
**Vary reflectivity with sun position?** Yes  
**Correlate slope error with surface type?** Yes  
**Slope error:** 6.55 mrad

Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.892686	-117.279556	1560.93	0.00	1560.93
2	33.892704	-117.278215	1559.74	0.00	1559.74
3	33.891145	-117.278225	1558.97	0.00	1558.97
4	33.891145	-117.279523	1566.81	0.00	1566.81



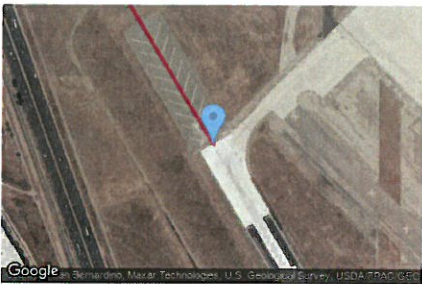
## 2-Mile Flight Path Receptor(s)

**Name:** FP 12  
**Description:**  
**Threshold height :** 50 ft  
**Direction:** 135.0 deg  
**Glide slope:** 3.0 deg  
**Pilot view restricted?** Yes  
**Vertical view restriction:** 30.0 deg  
**Azimuthal view restriction:** 50.0 deg



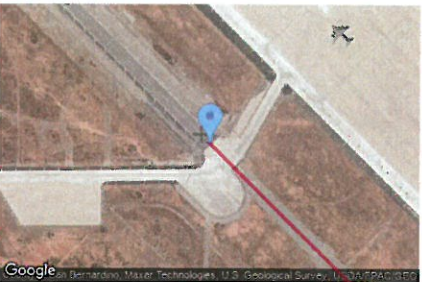
Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.890243	-117.260666	1519.07	50.00	1569.08
2-mile point	33.910687	-117.285323	1543.08	579.46	2122.53

**Name:** FP14  
**Description:**  
**Threshold height :** 56 ft  
**Direction:** 149.0 deg  
**Glide slope:** 2.59 deg  
**Pilot view restricted?** Yes  
**Vertical view restriction:** 30.0 deg  
**Azimuthal view restriction:** 50.0 deg



Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.896436	-117.270631	1536.71	56.00	1592.72
2-mile point	33.921216	-117.288597	1524.71	545.71	2070.42

**Name:** FP 30  
**Description:**  
**Threshold height :** 50 ft  
**Direction:** 315.0 deg  
**Glide slope:** 3.0 deg  
**Pilot view restricted?** Yes  
**Vertical view restriction:** 30.0 deg  
**Azimuthal view restriction:** 50.0 deg



Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.884351	-117.253579	1507.14	50.00	1557.15
2-mile point	33.863907	-117.228924	1469.94	640.66	2110.60

Name: FP 32

Description:

Threshold height : 59 ft

Direction: 329.0 deg

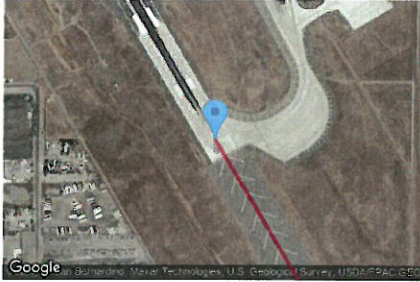
Glide slope: 3.0 deg

Pilot view restricted? Yes

Vertical view restriction: 30.0 deg

Azimuthal view restriction: 50.0 deg

Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.865319	-117.248518	1488.44	59.00	1547.45
2-mile point	33.840536	-117.230564	1460.06	640.84	2100.90



### Route Receptor(s)

**Name:** GA Rwy 12 Base  
**Route type** Two-way  
**View angle:** 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.910322	-117.264967	1500.14	1300.12	2800.27
2	33.905592	-117.270622	1500.14	1300.12	2800.27

**Name:** GA Rwy 12 Crosswind  
**Route type** Two-way  
**View angle:** 50.0 deg



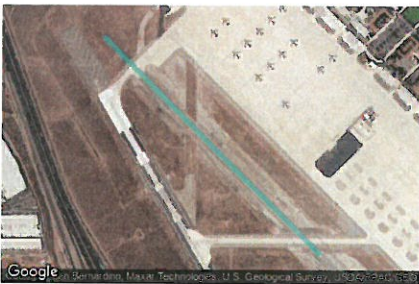
Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.876081	-117.235119	1500.14	1300.12	2800.27
2	33.880814	-117.229467	1500.14	1300.12	2800.27

**Name:** GA Rwy 12 Downwind  
**Route type** Two-way  
**View angle:** 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.887897	-117.229483	1500.14	1300.12	2800.27
2	33.910333	-117.256469	1500.14	1300.12	2800.27

**Name:** GA Rwy 12 Final  
**Route type** Two-way  
**View angle:** 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.898508	-117.270608	1500.14	1300.12	2800.27
2	33.890258	-117.260680	1500.14	0.00	1500.14

Name: GA Rwy 12 Upwind  
 Route type Two-way  
 View angle: 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.884319	-117.253536	1500.14	0.00	1500.14
2	33.876069	-117.243611	1500.14	1300.12	2800.27

Name: GA Rwy 30 Base  
 Route type Two-way  
 View angle: 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.880814	-117.229471	1500.14	1300.12	2800.27
2	33.876081	-117.235119	1500.14	1300.12	2800.27

Name: GA Rwy 30 Crosswind  
 Route type Two-way  
 View angle: 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.905592	-117.270622	1500.14	1300.12	2800.27
2	33.910322	-117.264967	1500.14	1300.12	2800.27

Name: GA Rwy 30 Downwind  
 Route type Two-way  
 View angle: 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.910333	-117.256469	1500.14	1300.12	2800.27
2	33.887897	-117.229483	1500.14	1300.12	2800.27

Name: GA Rwy 30 Final  
 Route type Two-way  
 View angle: 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.876069	-117.243611	1500.14	1300.13	2800.28
2	33.884319	-117.253536	1500.14	0.00	1500.14

Name: GA Rwy 30 Upwind  
 Route type Two-way  
 View angle: 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.890258	-117.260681	1500.14	0.00	1500.14
2	33.898508	-117.270608	1500.14	1300.12	2800.27

**Discrete Observation Receptors**

Number	Latitude	Longitude	Ground elevation	Height above ground	Total Elevation
	deg	deg	ft	ft	ft
1-ATCT	33.891568	-117.251182	1510.14	118.01	1628.15

1-ATCT map image



## Summary of PV Glare Analysis

*PV configuration and total predicted glare*

PV Name	Tilt	Orientation	"Green" Glare	"Yellow" Glare	Energy Produced	Data File
	deg	deg	min	min	kWh	
PV array 1	10.0	194.0	10,526	0	-	-

### Distinct glare per month

Excludes overlapping glare from PV array for multiple receptors at matching time(s)

PV	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec
pv-array-1 (green)	0	0	371	258	666	999	865	224	475	102	0	0
pv-array-1 (yellow)	0	0	0	0	0	0	0	0	0	0	0	0

## PV & Receptor Analysis Results

*Results for each PV array and receptor*

### PV array 1 low potential for temporary after-image

Component	Green glare (min)	Yellow glare (min)
FP: FP 12	0	0
FP: FP14	0	0
FP: FP 30	1641	0
FP: FP 32	0	0
OP: 1-ATCT	0	0
Route: GA Rwy 12 Base	0	0
Route: GA Rwy 12 Crosswind	0	0
Route: GA Rwy 12 Downwind	1022	0
Route: GA Rwy 12 Final	663	0
Route: GA Rwy 12 Upwind	2761	0
Route: GA Rwy 30 Base	0	0
Route: GA Rwy 30 Crosswind	0	0
Route: GA Rwy 30 Downwind	1011	0
Route: GA Rwy 30 Final	2765	0
Route: GA Rwy 30 Upwind	663	0

### PV array 1 - Receptor (FP 12)

*No glare found*

### PV array 1 - Receptor (FP14)

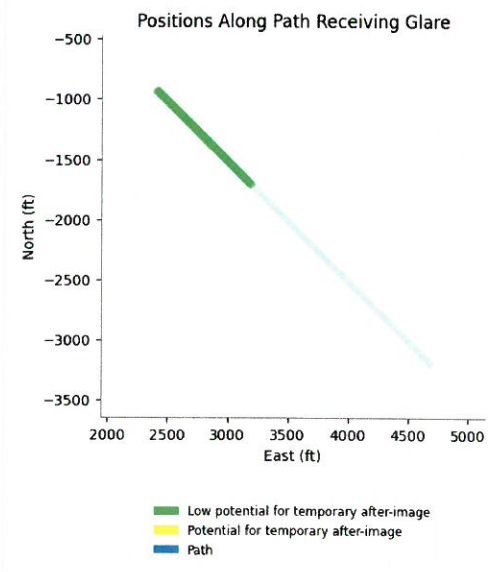
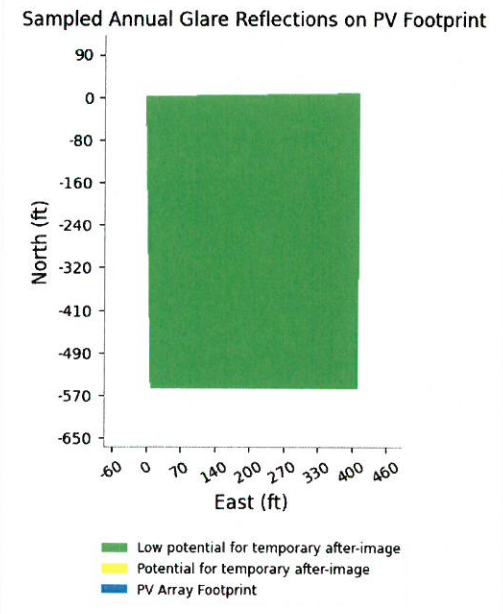
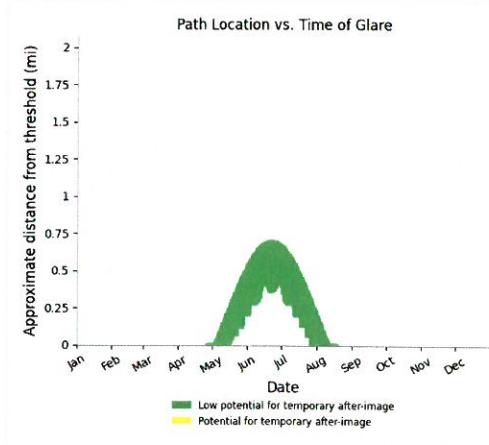
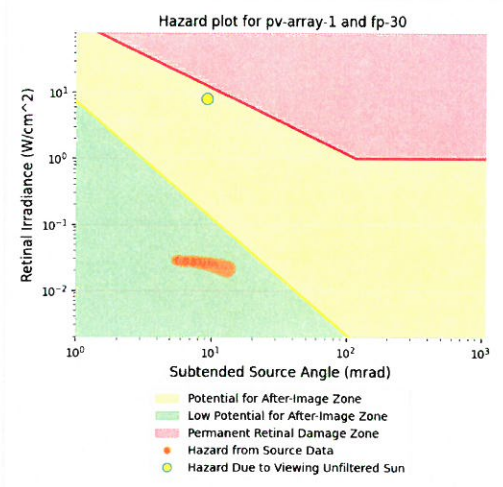
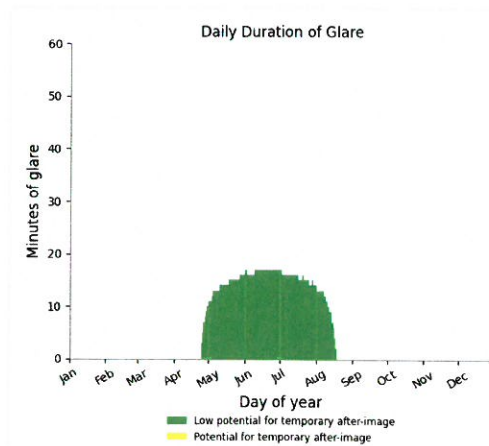
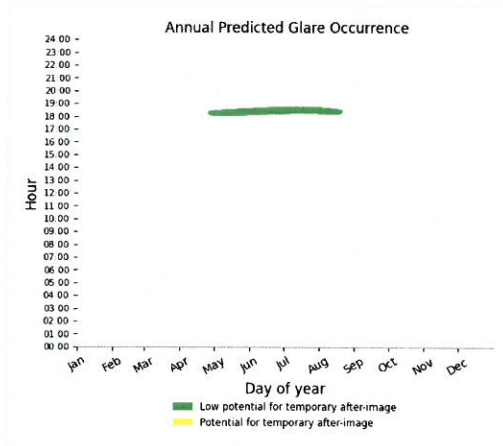
*No glare found*



### PV array 1 - Receptor (FP 30)

PV array is expected to produce the following glare for observers on this flight path:

- 1,641 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



**PV array 1 - Receptor (FP 32)**

*No glare found*

**PV array 1 - OP Receptor (1-ATCT)**

*No glare found*

**PV array 1 - Route Receptor (GA Rwy 12 Base)**

*No glare found*

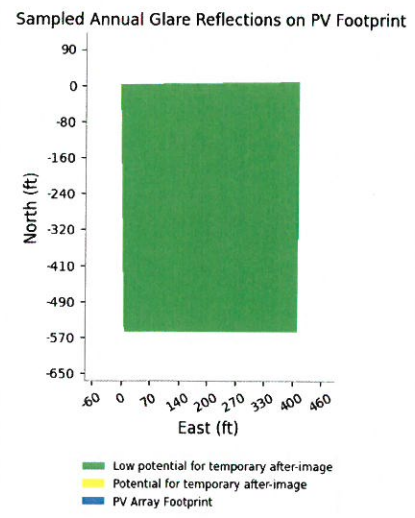
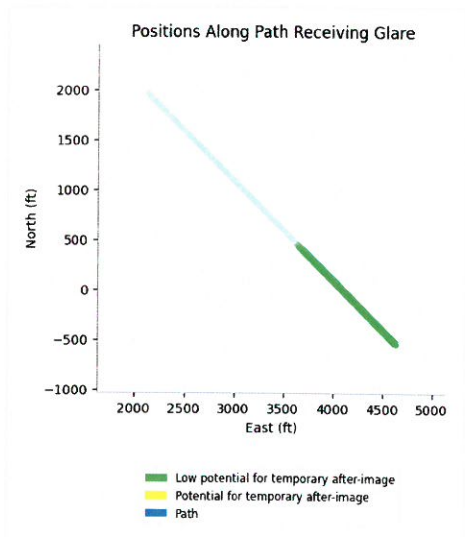
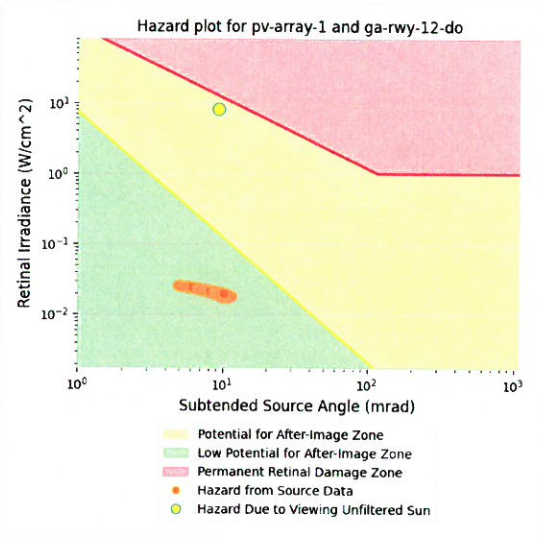
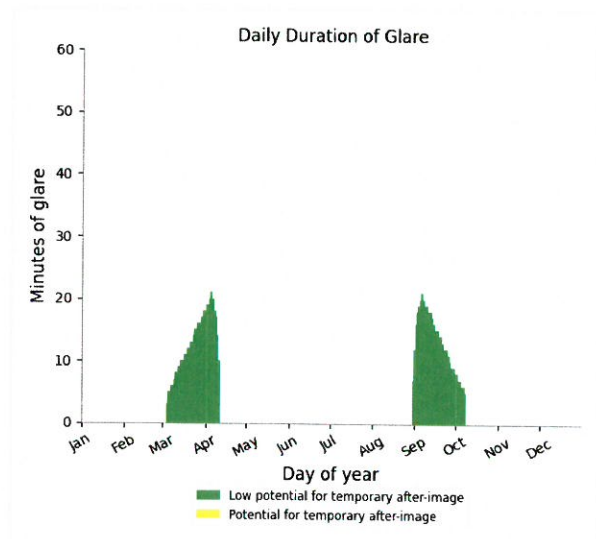
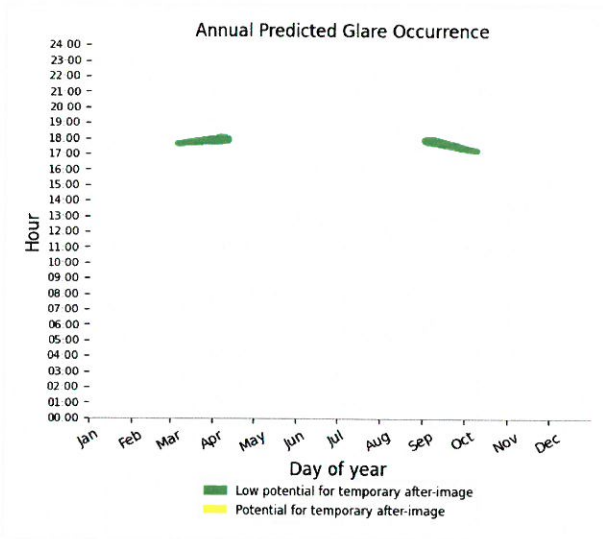
**PV array 1 - Route Receptor (GA Rwy 12 Crosswind )**

*No glare found*

### PV array 1 - Route Receptor (GA Rwy 12 Downwind)

PV array is expected to produce the following glare for receptors at this location:

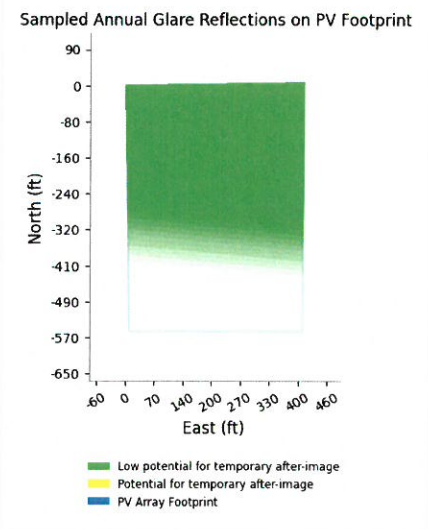
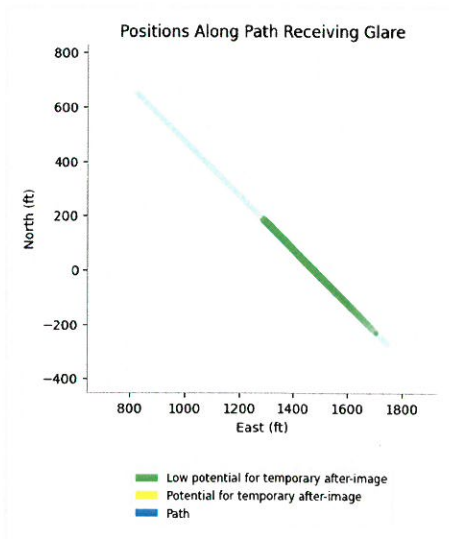
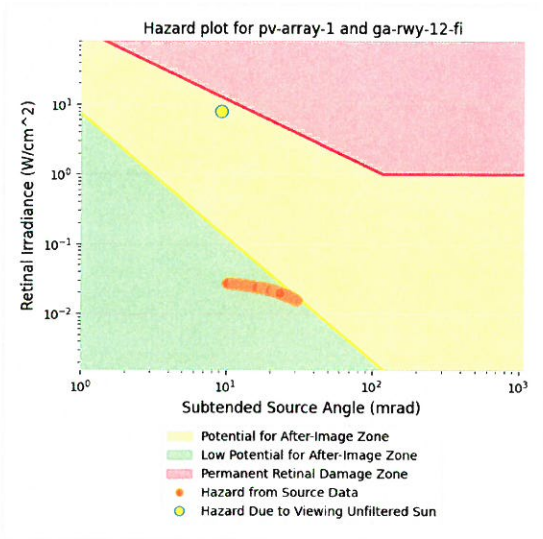
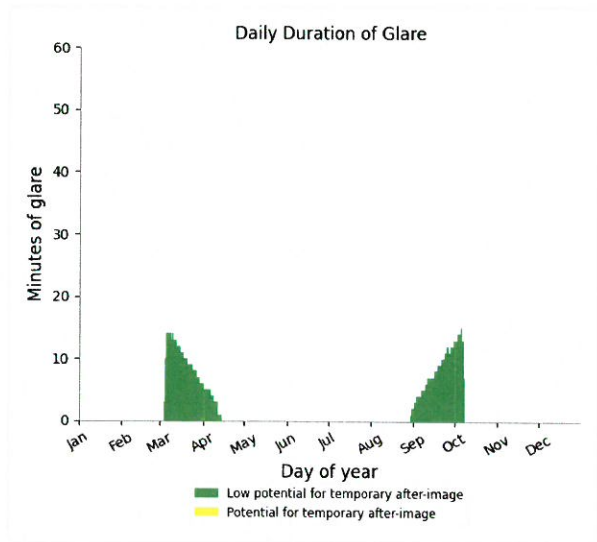
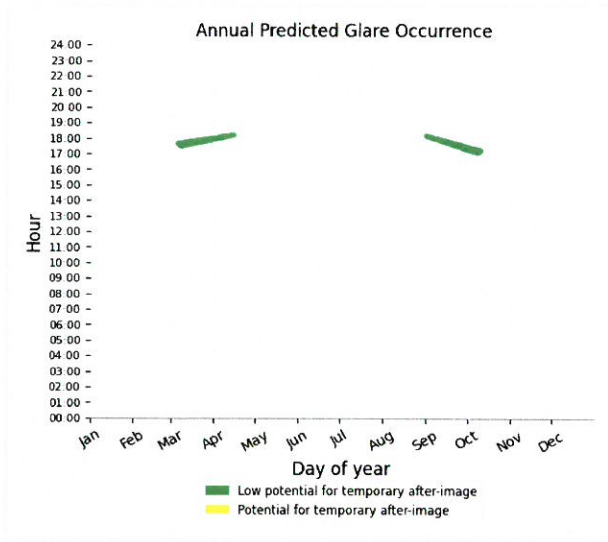
- 1,022 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



### PV array 1 - Route Receptor (GA Rwy 12 Final)

PV array is expected to produce the following glare for receptors at this location:

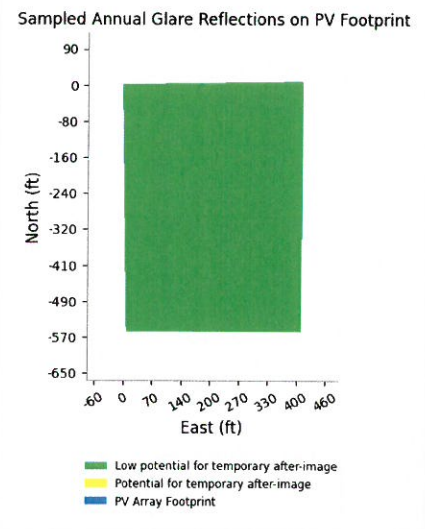
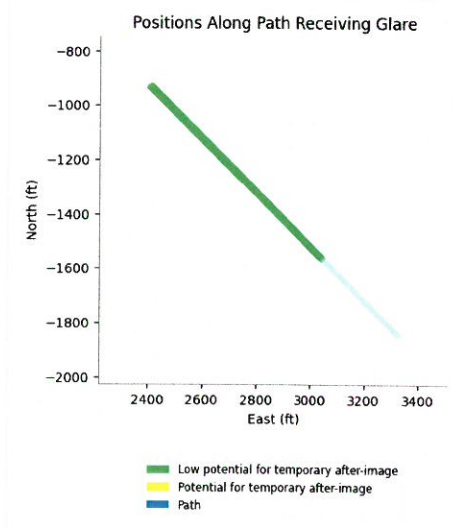
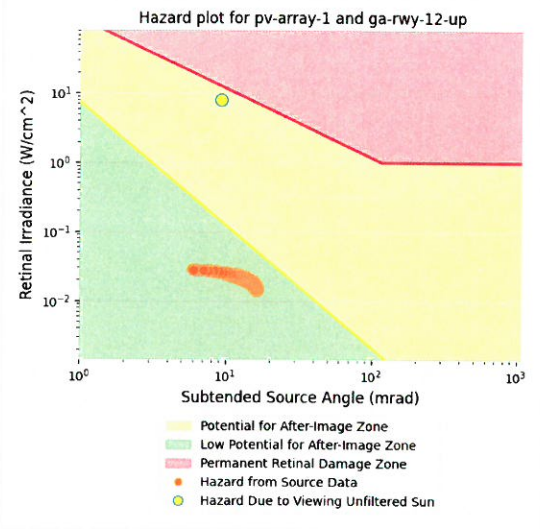
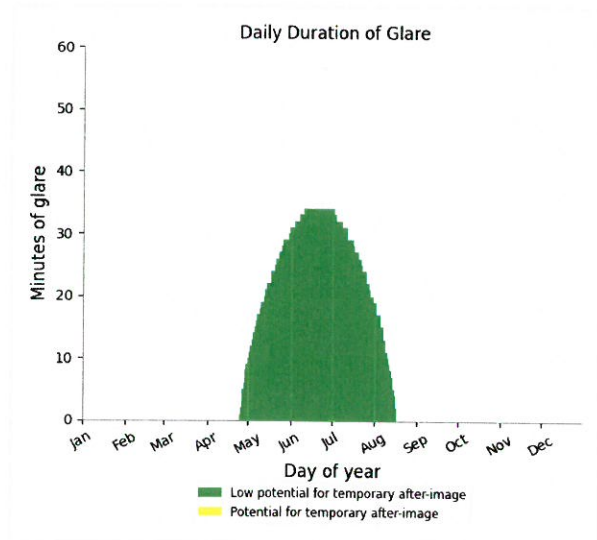
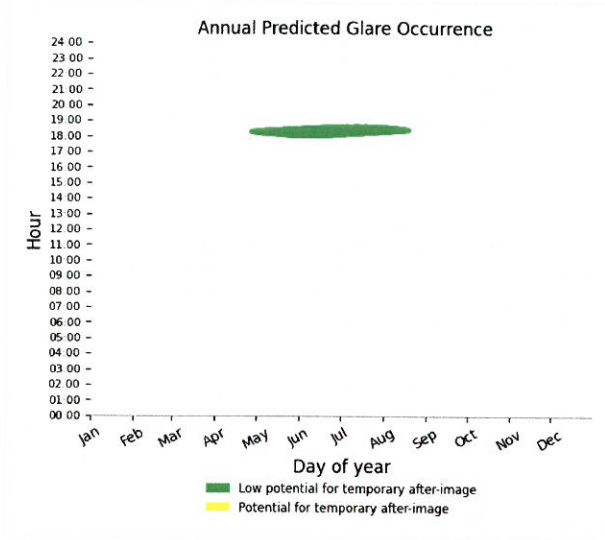
- 663 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



### PV array 1 - Route Receptor (GA Rwy 12 Upwind)

PV array is expected to produce the following glare for receptors at this location:

- 2,761 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



**PV array 1 - Route Receptor (GA Rwy 30 Base )**

*No glare found*

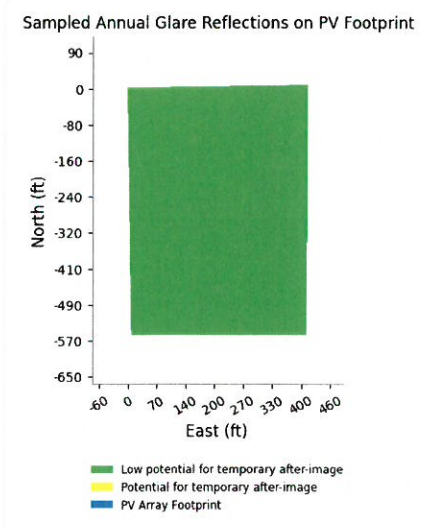
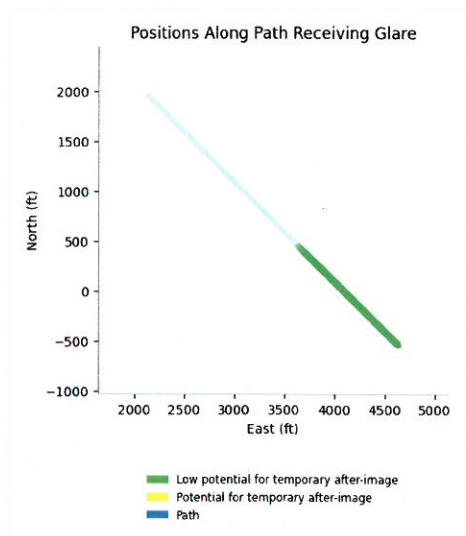
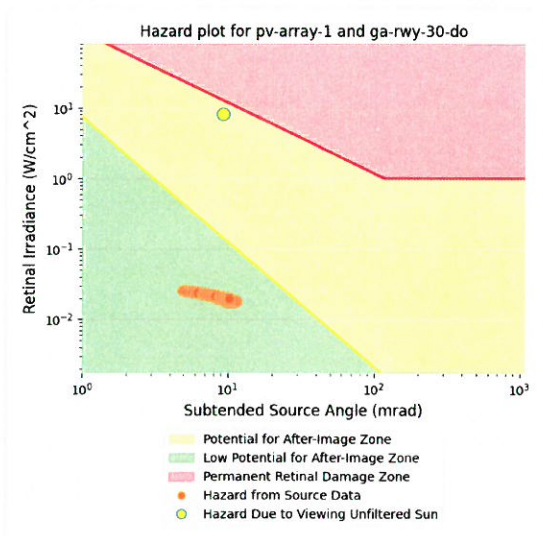
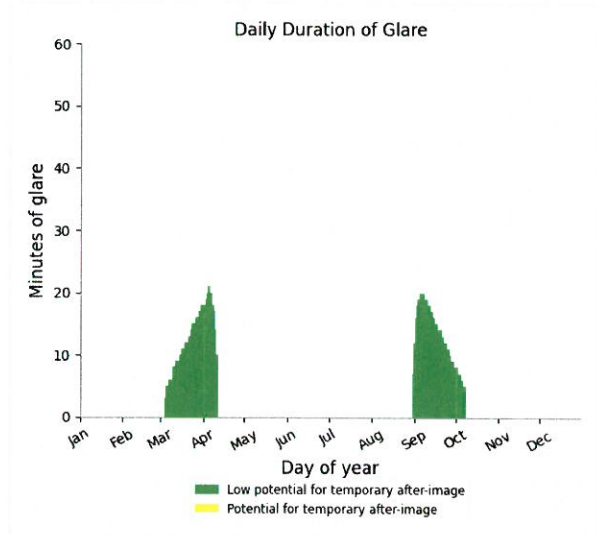
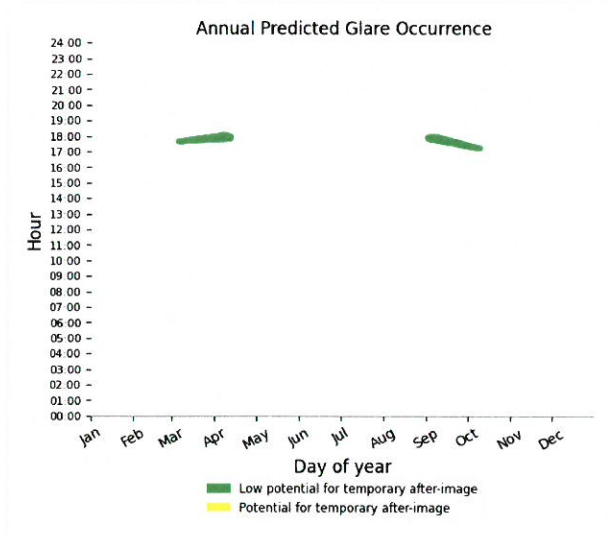
**PV array 1 - Route Receptor (GA Rwy 30 Crosswind)**

*No glare found*

### PV array 1 - Route Receptor (GA Rwy 30 Downwind)

PV array is expected to produce the following glare for receptors at this location:

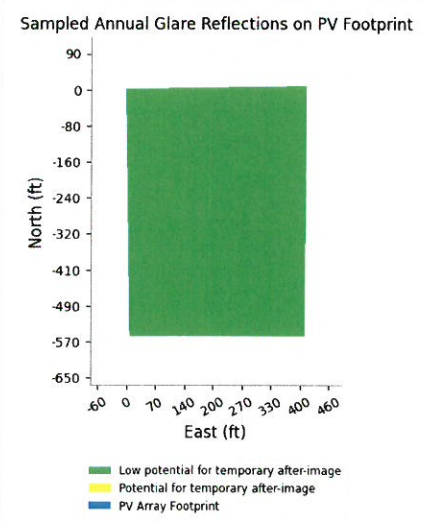
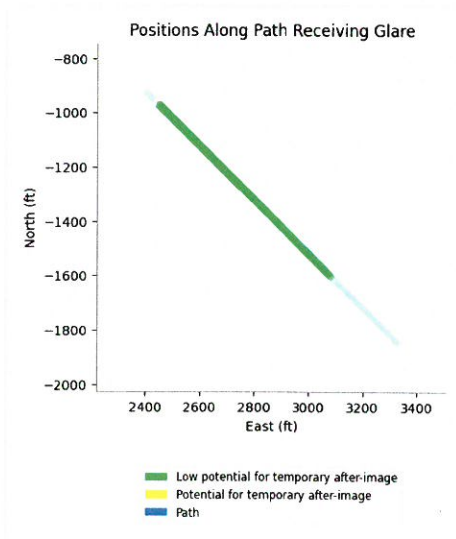
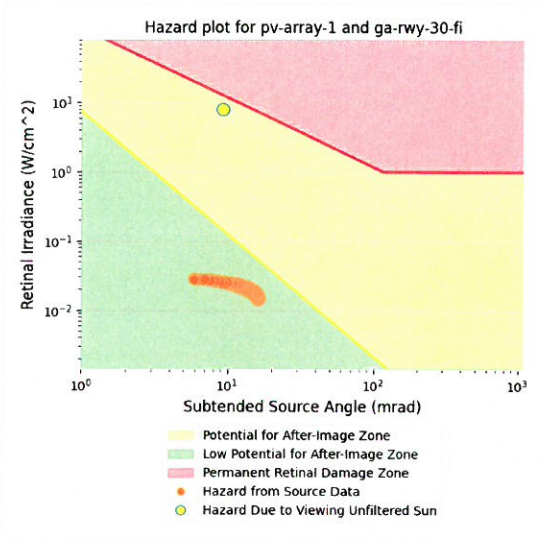
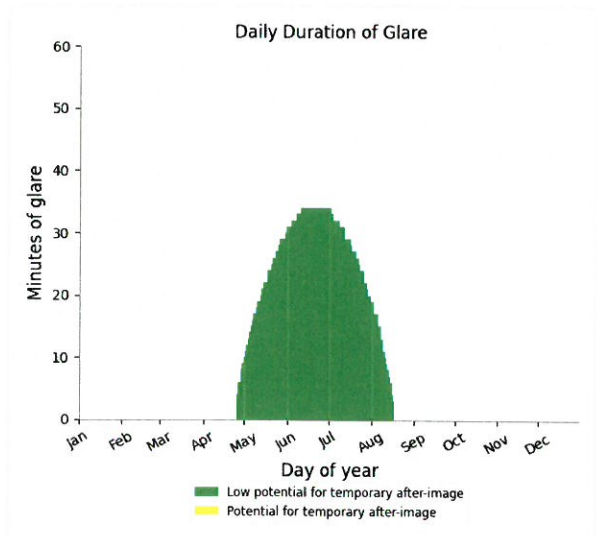
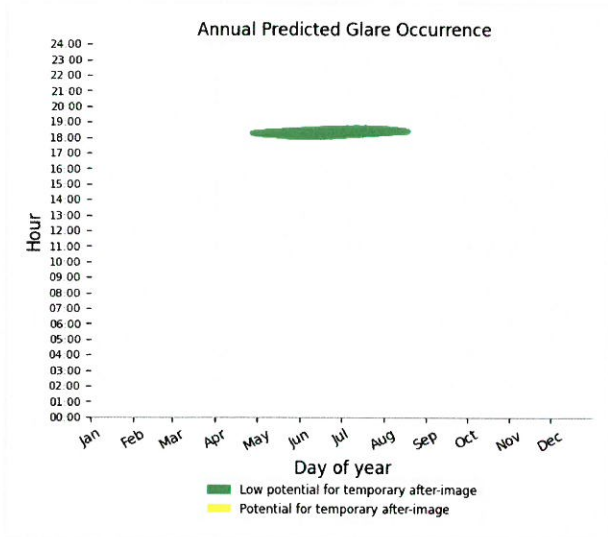
- 1,011 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



### PV array 1 - Route Receptor (GA Rwy 30 Final)

PV array is expected to produce the following glare for receptors at this location:

- 2,765 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.

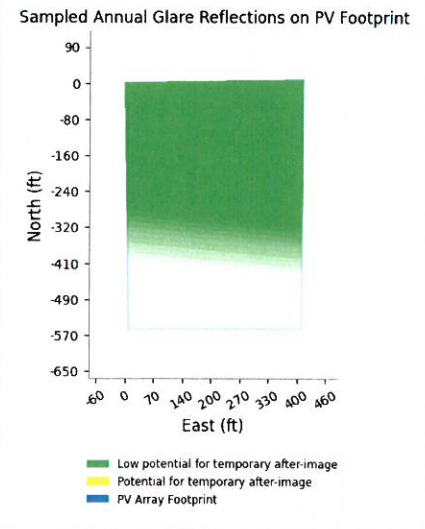
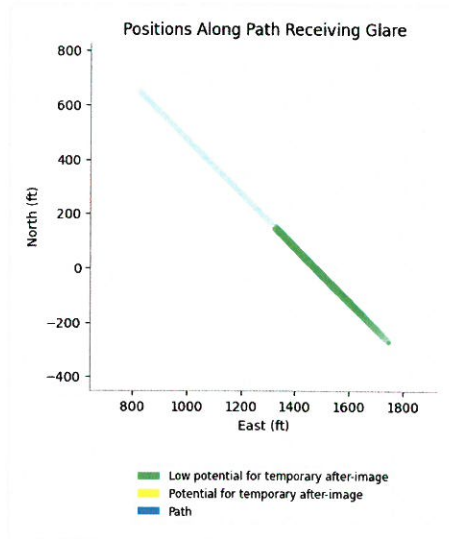
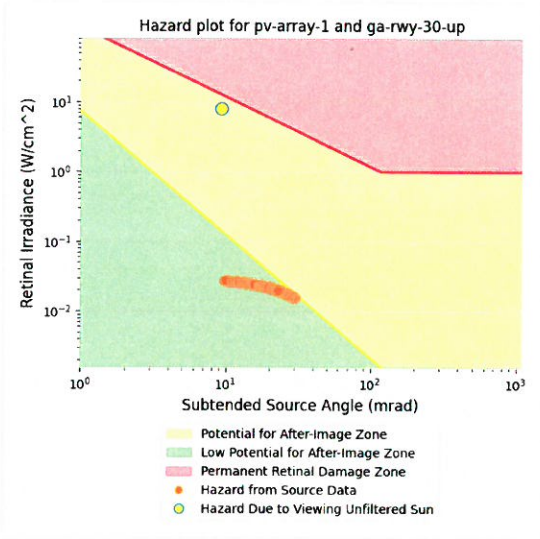
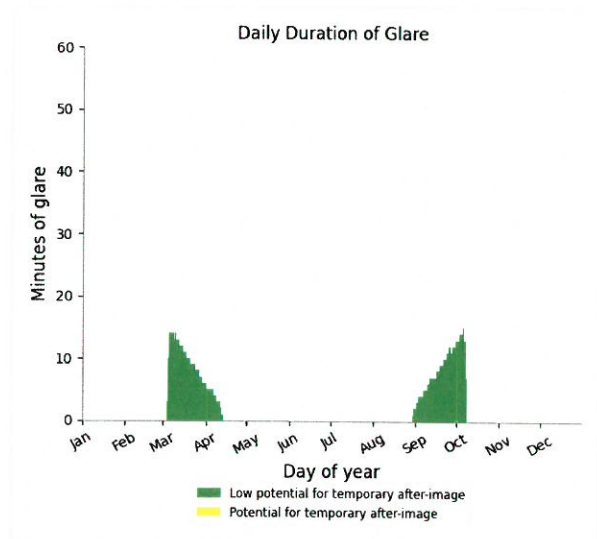
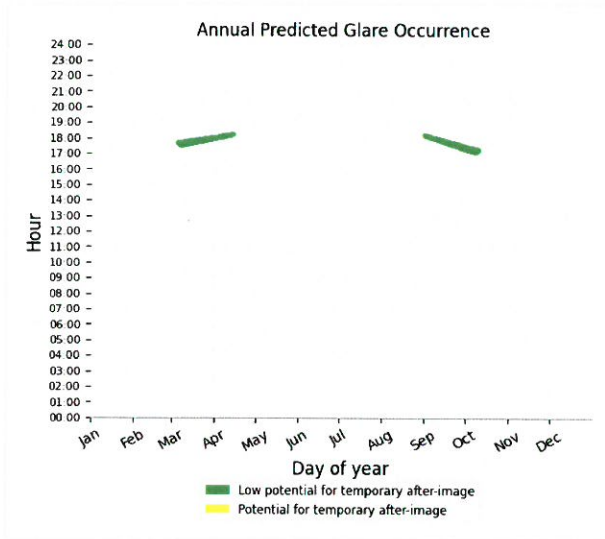




### PV array 1 - Route Receptor (GA Rwy 30 Upwind)

PV array is expected to produce the following glare for receptors at this location:

- 663 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



## Assumptions

- Times associated with glare are denoted in Standard time. For Daylight Savings, add one hour.
- Glare analyses do not account for physical obstructions between reflectors and receptors. This includes buildings, tree cover and geographic obstructions
- Detailed system geometry is not rigorously simulated.
- The glare hazard determination relies on several approximations including observer eye characteristics, angle of view, and typical blink response time. Actual values and results may vary.
- The system output calculation is a DNI-based approximation that assumes clear, sunny skies year-round. It should not be used in place of more rigorous modeling methods.
- Several V1 calculations utilize the PV array centroid, rather than the actual glare spot location, due to algorithm limitations. This may affect results for large PV footprints. Additional analyses of array sub-sections can provide additional information on expected glare.
- The subtended source angle (glare spot size) is constrained by the PV array footprint size. Partitioning large arrays into smaller sections will reduce the maximum potential subtended angle, potentially impacting results if actual glare spots are larger than the sub-array size. Additional analyses of the combined area of adjacent sub-arrays can provide more information on potential glare hazards. (See previous point on related limitations.)
- Hazard zone boundaries shown in the Glare Hazard plot are an approximation and visual aid. Actual ocular impact outcomes encompass a continuous, not discrete, spectrum.
- Glare locations displayed on receptor plots are approximate. Actual glare-spot locations may differ.
- Glare vector plots are simplified representations of analysis data. Actual glare emanations and results may differ.
- Refer to the **Help page** for detailed assumptions and limitations not listed here.

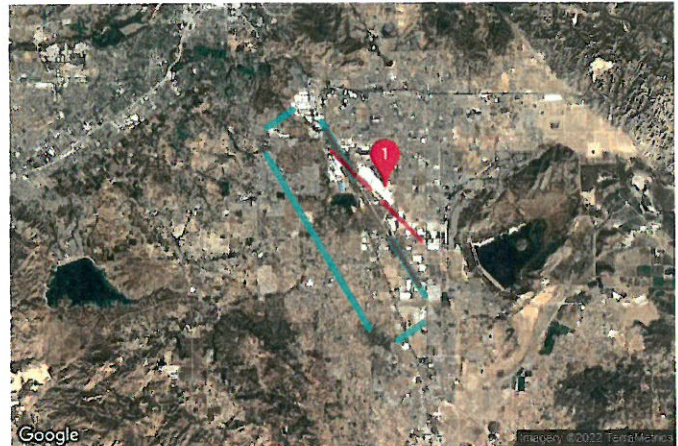


# Riverside CA 2

## Rwy 14 32 C-17 KC-135 Rectangular Analysis

Created March 4, 2022  
 Updated April 12, 2022  
 Time-step 1 minute  
 Timezone offset UTC-8  
 Site ID 65666.11191

Project type Basic  
 Project status: active  
 Category 500 kW to 1 MW  
 (1,000 kW / 8 acre limit)



### Misc. Analysis Settings

DNI: varies (1,000.0 W/m<sup>2</sup> peak)  
 Ocular transmission coefficient: 0.5  
 Pupil diameter: 0.002 m  
 Eye focal length: 0.017 m  
 Sun subtended angle: 9.3 mrad

### Analysis Methodologies:

- Observation point: **Version 2**
- 2-Mile Flight Path: **Version 2**
- Route: **Version 2**

## Summary of Results Glare with low potential for temporary after-image predicted

PV Name	Tilt	Orientation	"Green" Glare	"Yellow" Glare	Energy Produced
	deg	deg	min	min	kWh
PV array 1	10.0	194.0	6,855	0	-

# Component Data

## PV Array(s)

Total PV footprint area: 5.2 acres

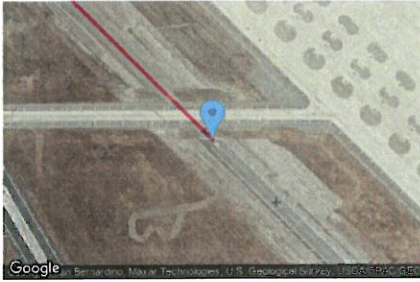
**Name:** PV array 1  
**Footprint area:** 5.2 acres  
**Axis tracking:** Fixed (no rotation)  
**Tilt:** 10.0 deg  
**Orientation:** 194.0 deg  
**Rated power:** -  
**Panel material:** Smooth glass without AR coating  
**Vary reflectivity with sun position?** Yes  
**Correlate slope error with surface type?** Yes  
**Slope error:** 6.55 mrad

Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.892686	-117.279556	1560.93	0.00	1560.93
2	33.892704	-117.278215	1559.74	0.00	1559.74
3	33.891145	-117.278225	1558.97	0.00	1558.97
4	33.891145	-117.279523	1566.81	0.00	1566.81



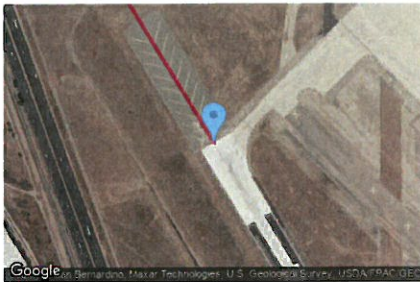
## 2-Mile Flight Path Receptor(s)

**Name:** FP 12  
**Description:**  
**Threshold height :** 50 ft  
**Direction:** 135.0 deg  
**Glide slope:** 3.0 deg  
**Pilot view restricted?** Yes  
**Vertical view restriction:** 30.0 deg  
**Azimuthal view restriction:** 50.0 deg



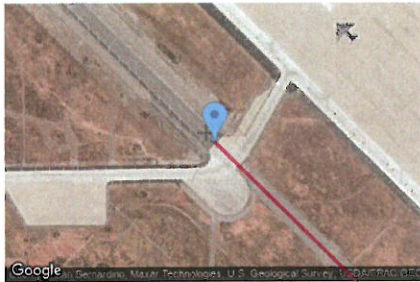
Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.890243	-117.260666	1519.07	50.00	1569.08
2-mile point	33.910687	-117.285323	1543.08	579.46	2122.53

**Name:** FP14  
**Description:**  
**Threshold height :** 56 ft  
**Direction:** 149.0 deg  
**Glide slope:** 2.59 deg  
**Pilot view restricted?** Yes  
**Vertical view restriction:** 30.0 deg  
**Azimuthal view restriction:** 50.0 deg



Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.896436	-117.270631	1536.71	56.00	1592.72
2-mile point	33.921216	-117.288597	1524.71	545.71	2070.42

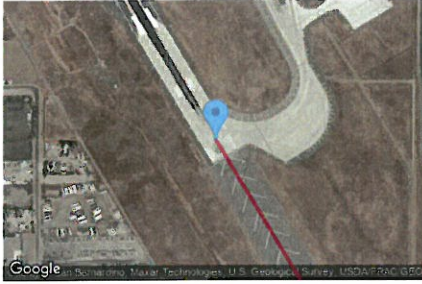
**Name:** FP 30  
**Description:**  
**Threshold height :** 50 ft  
**Direction:** 315.0 deg  
**Glide slope:** 3.0 deg  
**Pilot view restricted?** Yes  
**Vertical view restriction:** 30.0 deg  
**Azimuthal view restriction:** 50.0 deg



Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.884351	-117.253579	1507.14	50.00	1557.15
2-mile point	33.863907	-117.228924	1469.94	640.66	2110.60

**Name:** FP 32  
**Description:**  
**Threshold height :** 59 ft  
**Direction:** 329.0 deg  
**Glide slope:** 3.0 deg  
**Pilot view restricted?** Yes  
**Vertical view restriction:** 30.0 deg  
**Azimuthal view restriction:** 50.0 deg

Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.865319	-117.248518	1488.44	59.00	1547.45
2-mile point	33.840536	-117.230564	1460.06	640.84	2100.90



### Route Receptor(s)

Name: C KC Rwy 14 Base  
 Route type Two-way  
 View angle: 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.922394	-117.325047	1500.14	1500.14	3000.29
2	33.931244	-117.309014	1500.14	1500.14	3000.29

Name: C KC Rwy 14 Crosswind  
 Route type Two-way  
 View angle: 50.0 deg



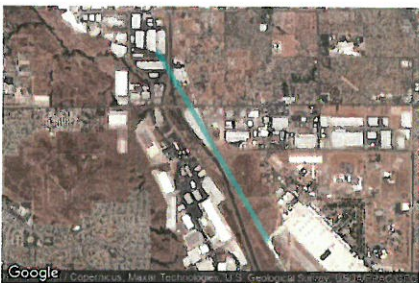
Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.821961	-117.228367	1500.14	1500.14	3000.29
2	33.813147	-117.244350	1500.14	1500.14	3000.29

Name: C KC Rwy 14 Downwind  
 Route type Two-way  
 View angle: 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.819225	-117.262269	1500.14	1500.14	3000.29
2	33.908131	-117.325528	1500.14	1500.14	3000.29

Name: C KC Rwy 14 Final  
 Route type Two-way  
 View angle: 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.925156	-117.291061	1500.14	1500.14	3000.29
2	33.896431	-117.270636	1500.14	0.00	1500.14

**Name:** C KC Rwy 14 Upwind  
**Route type:** Two-way  
**View angle:** 50.0 deg



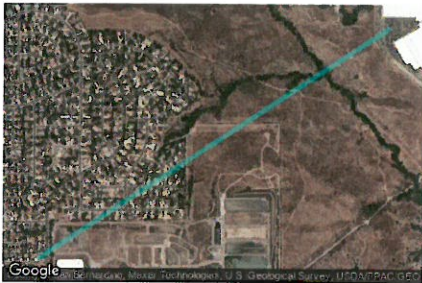
Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.864994	-117.248281	1500.14	0.00	1500.14
2	33.836269	-117.227869	1500.14	1500.14	3000.29

**Name:** C KC Rwy 32 Base  
**Route type:** Two-way  
**View angle:** 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.813147	-117.244350	1500.14	1500.14	3000.29
2	33.821961	-117.228367	1500.14	1500.14	3000.29

**Name:** C KC Rwy 32 Crosswind  
**Route type:** Two-way  
**View angle:** 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.931244	-117.309014	1500.14	1500.14	3000.29
2	33.922394	-117.325047	1500.14	1500.14	3000.29

**Name:** C KC Rwy 32 Downwind  
**Route type:** Two-way  
**View angle:** 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.908131	-117.325528	1500.14	1500.14	3000.29
2	33.819225	-117.262269	1500.14	1500.14	3000.29



Name: C KC Rwy 32 Final  
 Route type Two-way  
 View angle: 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.836269	-117.227869	1500.14	1500.14	3000.29
2	33.864994	-117.248281	1500.14	0.00	1500.14

Name: C KC Rwy 32 Upwind  
 Route type Two-way  
 View angle: 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.896431	-117.270636	1500.14	0.00	1500.14
2	33.925156	-117.291061	1500.14	1500.14	3000.29

### Discrete Observation Receptors

Number	Latitude	Longitude	Ground elevation	Height above ground	Total Elevation
	deg	deg	ft	ft	ft
1-ATCT	33.891568	-117.251182	1510.14	118.01	1628.15

1-ATCT map image



## Summary of PV Glare Analysis

*PV configuration and total predicted glare*

PV Name	Tilt	Orientation	"Green" Glare	"Yellow" Glare	Energy Produced	Data File
	deg	deg	min	min	kWh	
PV array 1	10.0	194.0	6,855	0	-	-

### Distinct glare per month

Excludes overlapping glare from PV array for multiple receptors at matching time(s)

PV	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec
pv-array-1 (green)	351	633	323	42	434	502	479	184	69	696	547	0
pv-array-1 (yellow)	0	0	0	0	0	0	0	0	0	0	0	0

## PV & Receptor Analysis Results

*Results for each PV array and receptor*

### PV array 1 low potential for temporary after-image

Component	Green glare (min)	Yellow glare (min)
FP: FP 12	0	0
FP: FP14	0	0
FP: FP 30	1641	0
FP: FP 32	0	0
OP: 1-ATCT	0	0
Route: C KC Rwy 14 Base	0	0
Route: C KC Rwy 14 Crosswind	0	0
Route: C KC Rwy 14 Downwind	2597	0
Route: C KC Rwy 14 Final	0	0
Route: C KC Rwy 14 Upwind	0	0
Route: C KC Rwy 32 Base	0	0
Route: C KC Rwy 32 Crosswind	0	0
Route: C KC Rwy 32 Downwind	2617	0
Route: C KC Rwy 32 Final	0	0
Route: C KC Rwy 32 Upwind	0	0

### PV array 1 - Receptor (FP 12)

*No glare found*

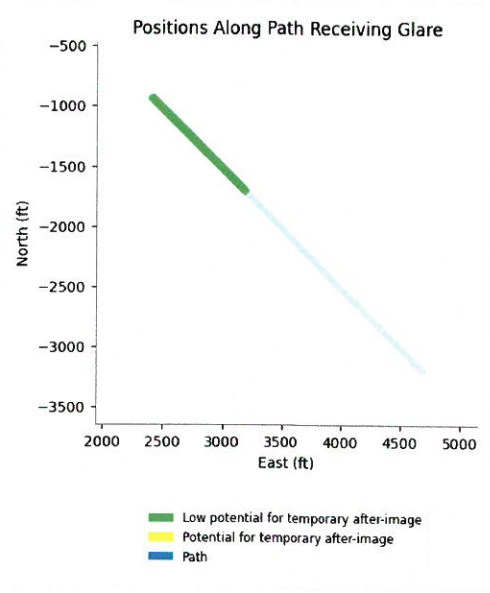
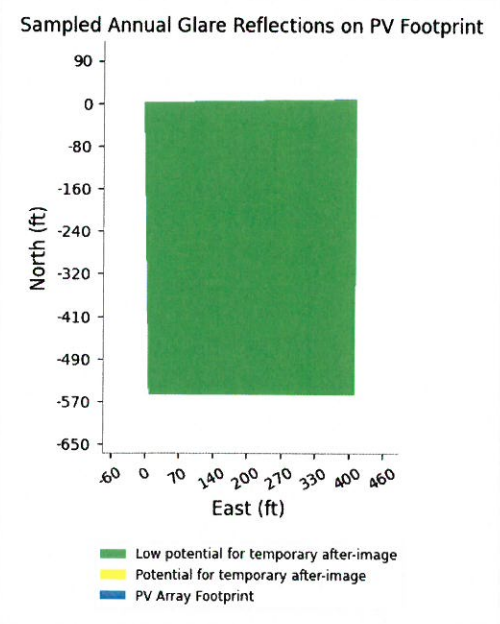
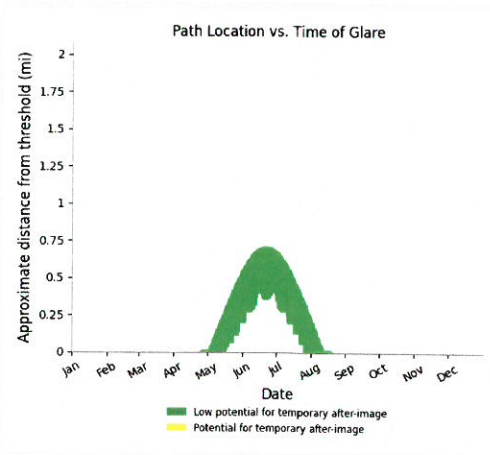
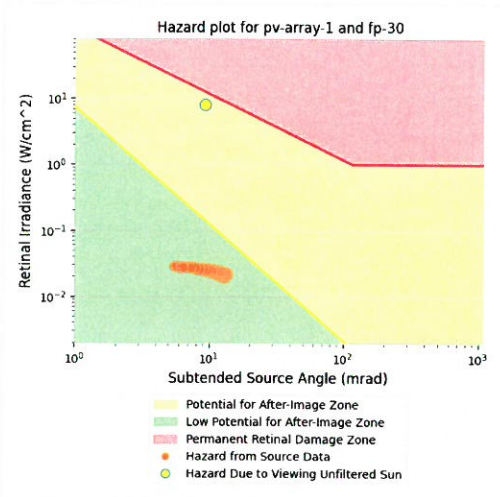
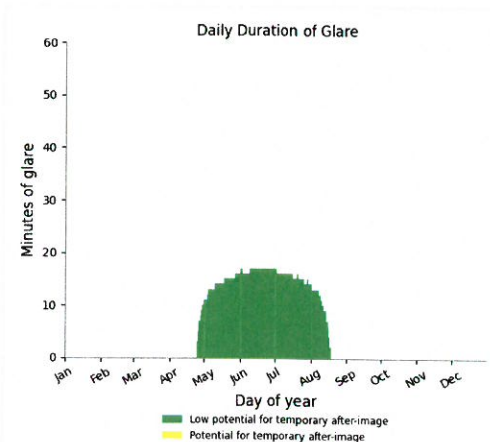
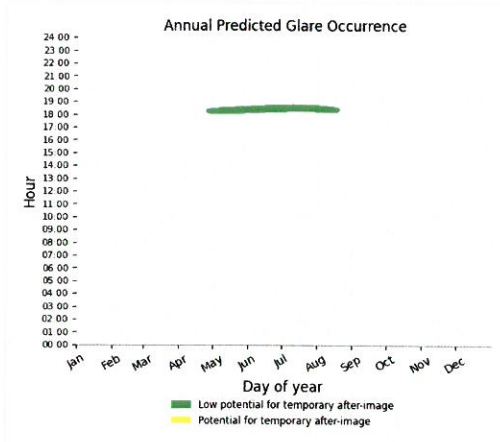
### PV array 1 - Receptor (FP14)

*No glare found*

### PV array 1 - Receptor (FP 30)

PV array is expected to produce the following glare for observers on this flight path:

- 1,641 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



**PV array 1 - Receptor (FP 32)**

*No glare found*

**PV array 1 - OP Receptor (1-ATCT)**

*No glare found*

**PV array 1 - Route Receptor (C KC Rwy 14 Base )**

*No glare found*

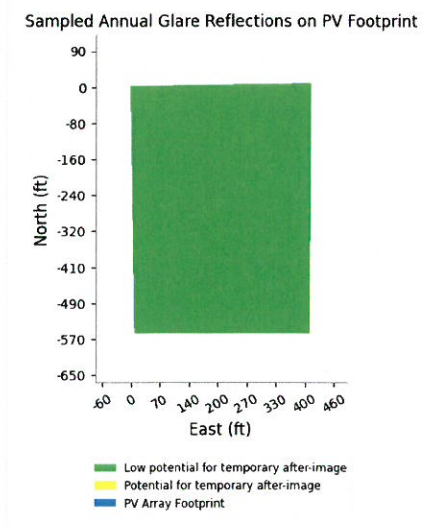
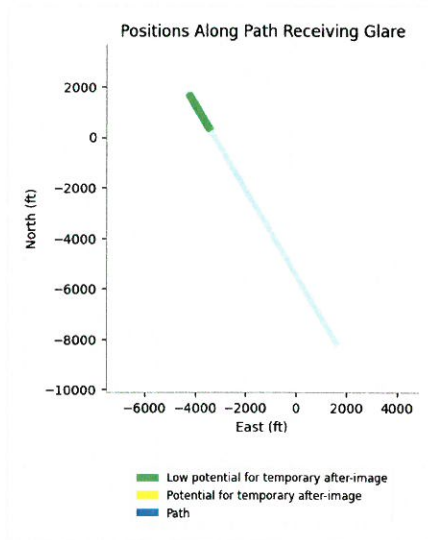
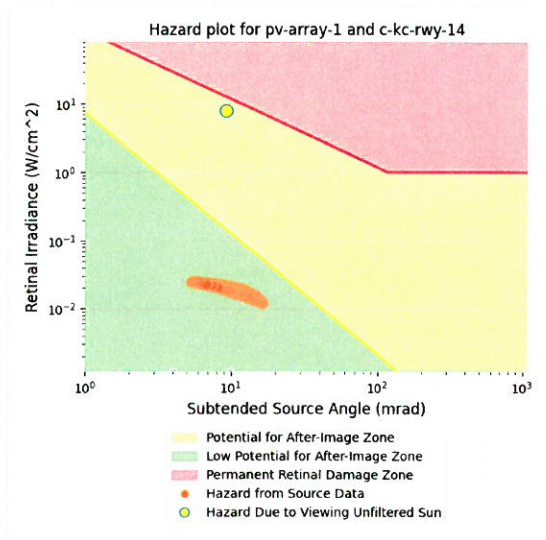
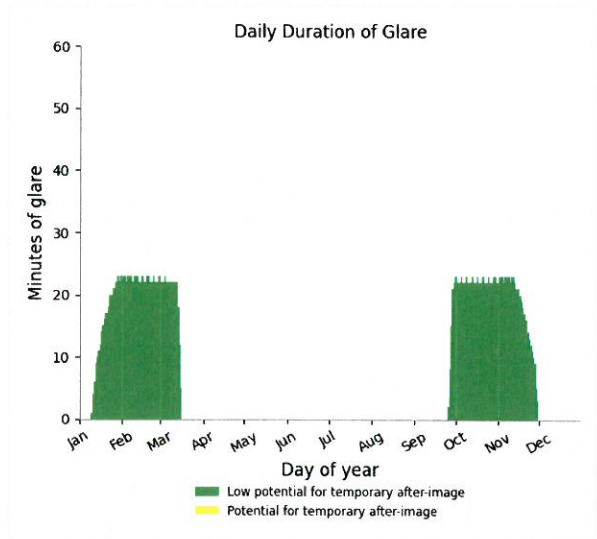
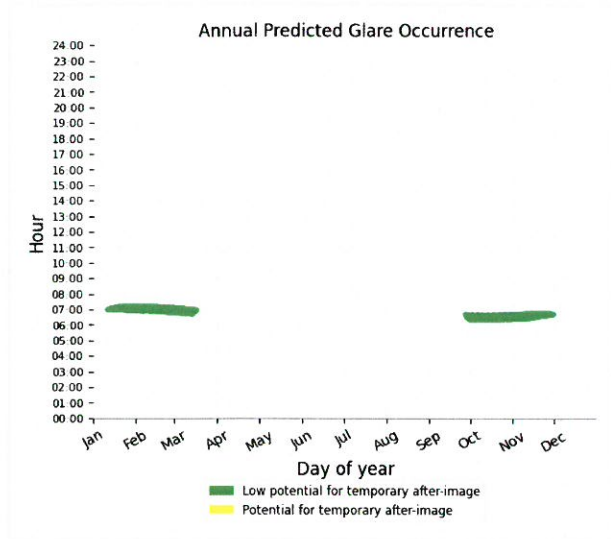
**PV array 1 - Route Receptor (C KC Rwy 14 Crosswind)**

*No glare found*

### PV array 1 - Route Receptor (C KC Rwy 14 Downwind )

PV array is expected to produce the following glare for receptors at this location:

- 2,597 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



**PV array 1 - Route Receptor (C KC Rwy 14 Final)**

*No glare found*

**PV array 1 - Route Receptor (C KC Rwy 14 Upwind )**

*No glare found*

**PV array 1 - Route Receptor (C KC Rwy 32 Base)**

*No glare found*

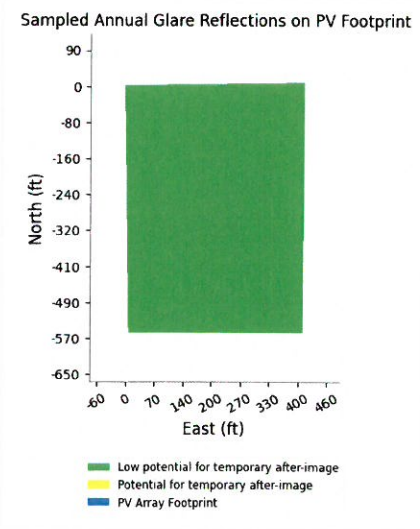
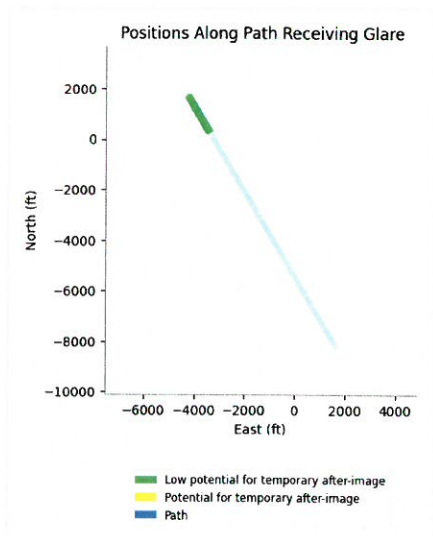
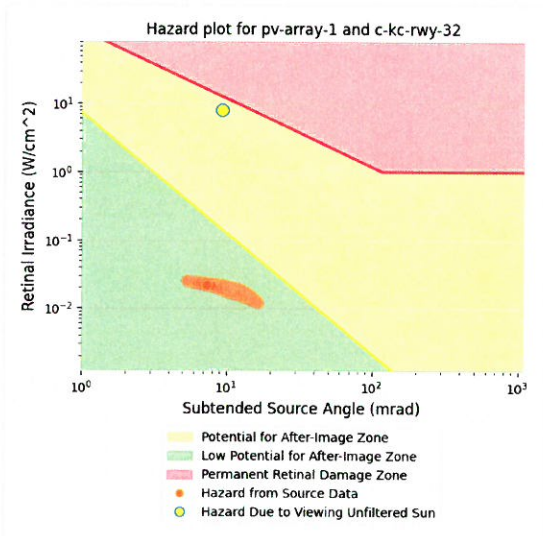
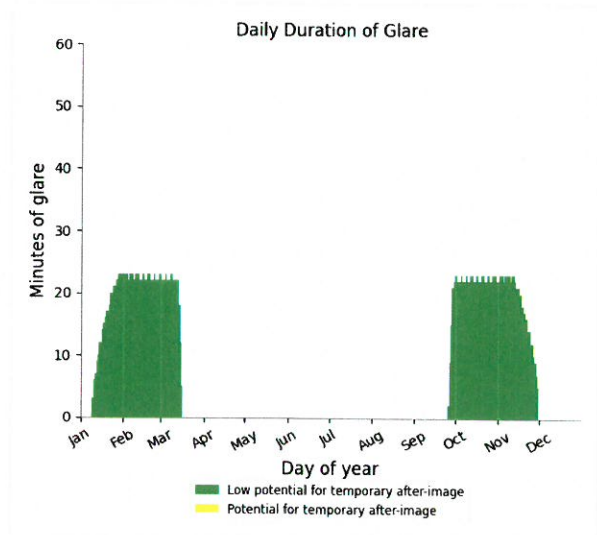
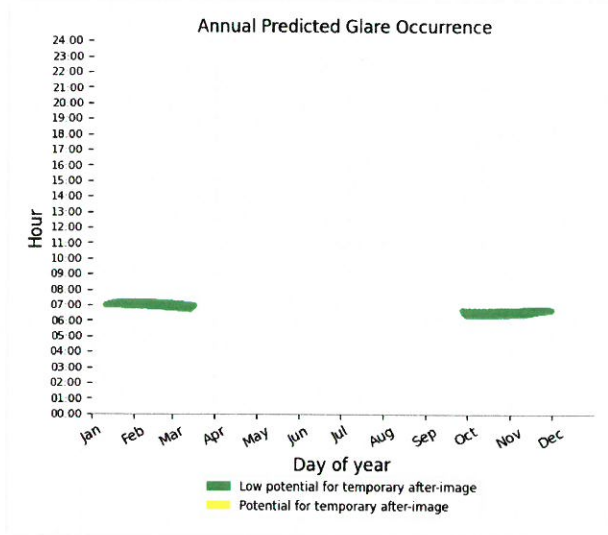
**PV array 1 - Route Receptor (C KC Rwy 32 Crosswind )**

*No glare found*

### PV array 1 - Route Receptor (C KC Rwy 32 Downwind )

PV array is expected to produce the following glare for receptors at this location:

- 2,617 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



## PV array 1 - Route Receptor ( C KC Rwy 32 Final)

*No glare found*

## PV array 1 - Route Receptor (C KC Rwy 32 Upwind)

*No glare found*

## Assumptions

- Times associated with glare are denoted in Standard time. For Daylight Savings, add one hour.
- Glare analyses do not account for physical obstructions between reflectors and receptors. This includes buildings, tree cover and geographic obstructions
- Detailed system geometry is not rigorously simulated.
- The glare hazard determination relies on several approximations including observer eye characteristics, angle of view, and typical blink response time. Actual values and results may vary.
- The system output calculation is a DNI-based approximation that assumes clear, sunny skies year-round. It should not be used in place of more rigorous modeling methods.
- Several V1 calculations utilize the PV array centroid, rather than the actual glare spot location, due to algorithm limitations. This may affect results for large PV footprints. Additional analyses of array sub-sections can provide additional information on expected glare.
- The subtended source angle (glare spot size) is constrained by the PV array footprint size. Partitioning large arrays into smaller sections will reduce the maximum potential subtended angle, potentially impacting results if actual glare spots are larger than the sub-array size. Additional analyses of the combined area of adjacent sub-arrays can provide more information on potential glare hazards. (See previous point on related limitations.)
- Hazard zone boundaries shown in the Glare Hazard plot are an approximation and visual aid. Actual ocular impact outcomes encompass a continuous, not discrete, spectrum.
- Glare locations displayed on receptor plots are approximate. Actual glare-spot locations may differ.
- Glare vector plots are simplified representations of analysis data. Actual glare emanations and results may differ.
- Refer to the **Help page** for detailed assumptions and limitations not listed here.





# Riverside CA 2

## Rwy 14 32 GA Rectangular Analysis

**Created** March 3, 2022  
**Updated** April 12, 2022  
**Time-step** 1 minute  
**Timezone offset** UTC-8  
**Site ID** 67485.11191

**Project type** Basic  
**Project status:** active  
**Category** 500 kW to 1 MW  
**(1,000 kW / 8 acre limit)**



### Misc. Analysis Settings

**DNI:** varies (1,000.0 W/m<sup>2</sup> peak)  
**Ocular transmission coefficient:** 0.5  
**Pupil diameter:** 0.002 m  
**Eye focal length:** 0.017 m  
**Sun subtended angle:** 9.3 mrad

### Analysis Methodologies:

- Observation point: **Version 2**
- 2-Mile Flight Path: **Version 2**
- Route: **Version 2**

## Summary of Results Glare with low potential for temporary after-image predicted

PV Name	Tilt	Orientation	"Green" Glare	"Yellow" Glare	Energy Produced
	deg	deg	min	min	kWh
PV array 1	10.0	194.0	10,442	0	-

# Component Data

## PV Array(s)

Total PV footprint area: 5.2 acres

Name: PV array 1

Footprint area: 5.2 acres

Axis tracking: Fixed (no rotation)

Tilt: 10.0 deg

Orientation: 194.0 deg

Rated power: -

Panel material: Smooth glass without AR coating

Vary reflectivity with sun position? Yes

Correlate slope error with surface type? Yes

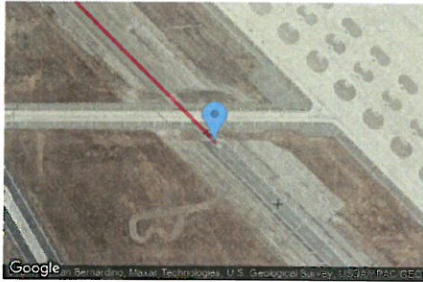
Slope error: 6.55 mrad

Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.892686	-117.279556	1560.93	0.00	1560.93
2	33.892704	-117.278215	1559.74	0.00	1559.74
3	33.891145	-117.278225	1558.97	0.00	1558.97
4	33.891145	-117.279523	1566.81	0.00	1566.81

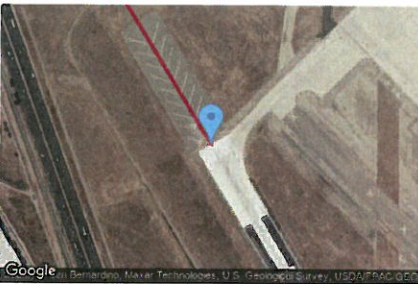


## 2-Mile Flight Path Receptor(s)

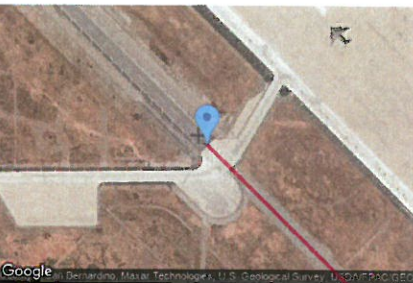
**Name:** FP 12  
**Description:**  
**Threshold height :** 50 ft  
**Direction:** 135.0 deg  
**Glide slope:** 3.0 deg  
**Pilot view restricted?** Yes  
**Vertical view restriction:** 30.0 deg  
**Azimuthal view restriction:** 50.0 deg



**Name:** FP14  
**Description:**  
**Threshold height :** 56 ft  
**Direction:** 149.0 deg  
**Glide slope:** 2.59 deg  
**Pilot view restricted?** Yes  
**Vertical view restriction:** 30.0 deg  
**Azimuthal view restriction:** 50.0 deg



**Name:** FP 30  
**Description:**  
**Threshold height :** 50 ft  
**Direction:** 315.0 deg  
**Glide slope:** 3.0 deg  
**Pilot view restricted?** Yes  
**Vertical view restriction:** 30.0 deg  
**Azimuthal view restriction:** 50.0 deg



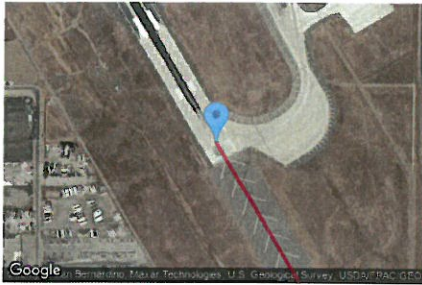
Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.890243	-117.260666	1519.07	50.00	1569.08
2-mile point	33.910687	-117.285323	1543.08	579.46	2122.53

Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.896436	-117.270631	1536.71	56.00	1592.72
2-mile point	33.921216	-117.288597	1524.71	545.71	2070.42

Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.884351	-117.253579	1507.14	50.00	1557.15
2-mile point	33.863907	-117.228924	1469.94	640.66	2110.60

**Name:** FP 32  
**Description:**  
**Threshold height :** 59 ft  
**Direction:** 329.0 deg  
**Glide slope:** 3.0 deg  
**Pilot view restricted?** Yes  
**Vertical view restriction:** 30.0 deg  
**Azimuthal view restriction:** 50.0 deg

Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.865319	-117.248518	1488.44	59.00	1547.45
2-mile point	33.840536	-117.230564	1460.06	640.84	2100.90



### Route Receptor(s)

**Name:** GA Rwy 14 Base  
**Route type:** Two-way  
**View angle:** 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.904833	-117.292903	1500.14	1500.14	3000.29
2	33.908242	-117.286017	1500.14	1500.14	3000.29

**Name:** GA Rwy 14 Crosswind  
**Route type:** Two-way  
**View angle:** 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.848078	-117.243236	1500.14	1500.14	3000.29
2	33.844669	-117.250119	1500.14	1500.14	3000.29

**Name:** GA Rwy 14 Downwind  
**Route type:** Two-way  
**View angle:** 50.0 deg



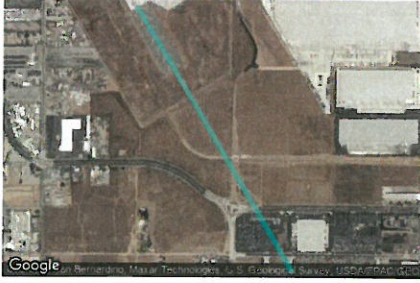
Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.846422	-117.258344	1500.14	1500.14	3000.29
2	33.897972	-117.295011	1500.14	1500.14	3000.29

**Name:** GA Rwy 14 Final  
**Route type:** Two-way  
**View angle:** 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.906486	-117.277783	1500.14	1500.14	3000.29
2	33.896431	-117.270636	1500.14	0.00	1500.14

**Name:** GA Rwy 14 Upwind  
**Route type:** Two-way  
**View angle:** 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.864994	-117.248281	1500.14	0.00	1500.14
2	33.854942	-117.241136	1500.14	1500.14	3000.29

**Name:** GA Rwy 32 Base  
**Route type:** Two-way  
**View angle:** 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.844669	-117.250119	1500.14	1500.14	3000.29
2	33.848078	-117.243236	1500.14	1500.14	3000.29

**Name:** GA Rwy 32 Crosswind  
**Route type:** Two-way  
**View angle:** 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.908242	-117.286017	1500.14	1500.14	3000.29
2	33.904833	-117.292903	1500.14	1500.14	3000.29

**Name:** GA Rwy 32 Downwind  
**Route type:** Two-way  
**View angle:** 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.897972	-117.295011	1500.14	1500.14	3000.29
2	33.846422	-117.258344	1500.14	1500.14	3000.29

**Name:** GA Rwy 32 Final  
**Route type:** Two-way  
**View angle:** 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.854942	-117.241136	1500.14	1500.14	3000.29
2	33.864994	-117.248281	1500.14	0.00	1500.14

**Name:** GA Rwy 32 Upwind  
**Route type:** Two-way  
**View angle:** 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.896431	-117.270636	1500.14	0.00	1500.14
2	33.906486	-117.277783	1500.14	1500.14	3000.29

### Discrete Observation Receptors

Number	Latitude	Longitude	Ground elevation	Height above ground	Total Elevation
	deg	deg	ft	ft	ft
1-ATCT	33.891568	-117.251182	1510.14	118.01	1628.15

1-ATCT map image



## Summary of PV Glare Analysis

*PV configuration and total predicted glare*

PV Name	Tilt	Orientation	"Green" Glare	"Yellow" Glare	Energy Produced	Data File
	deg	deg	min	min	kWh	
PV array 1	10.0	194.0	10,442	0	-	-

### Distinct glare per month

Excludes overlapping glare from PV array for multiple receptors at matching time(s)

PV	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec
pv-array-1 (green)	536	885	788	42	434	502	479	184	396	1025	755	33
pv-array-1 (yellow)	0	0	0	0	0	0	0	0	0	0	0	0

## PV & Receptor Analysis Results

*Results for each PV array and receptor*

### PV array 1 low potential for temporary after-image

Component	Green glare (min)	Yellow glare (min)
FP: FP 12	0	0
FP: FP14	0	0
FP: FP 30	1641	0
FP: FP 32	0	0
OP: 1-ATCT	0	0
Route: GA Rwy 14 Base	0	0
Route: GA Rwy 14 Crosswind	0	0
Route: GA Rwy 14 Downwind	4399	0
Route: GA Rwy 14 Final	0	0
Route: GA Rwy 14 Upwind	0	0
Route: GA Rwy 32 Base	0	0
Route: GA Rwy 32 Crosswind	0	0
Route: GA Rwy 32 Downwind	4402	0
Route: GA Rwy 32 Final	0	0
Route: GA Rwy 32 Upwind	0	0

### PV array 1 - Receptor (FP 12)

*No glare found*

### PV array 1 - Receptor (FP14)

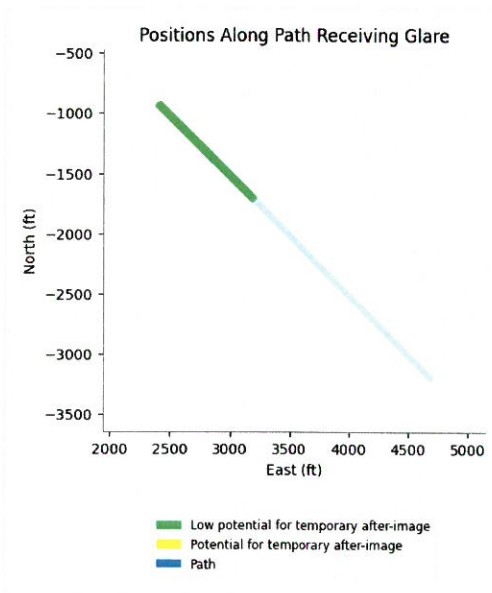
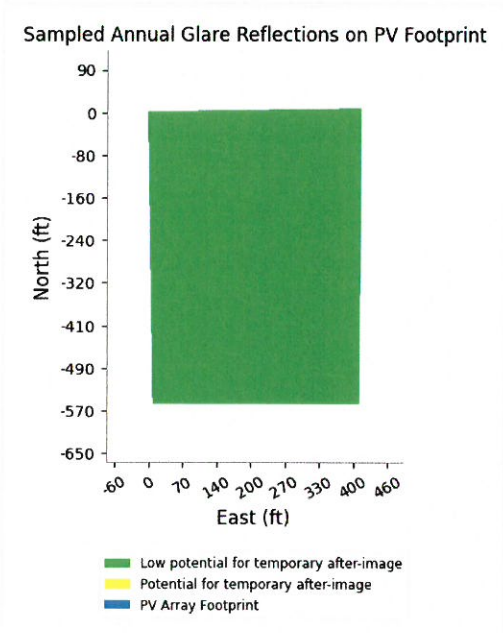
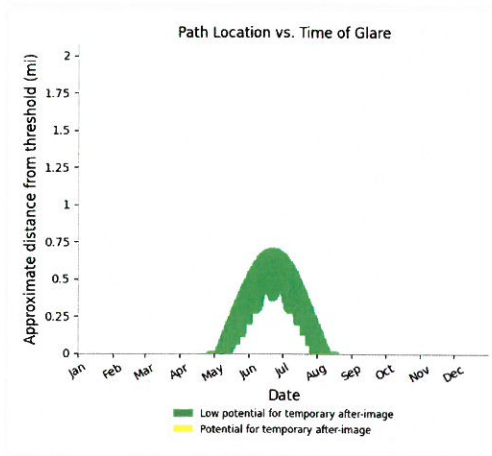
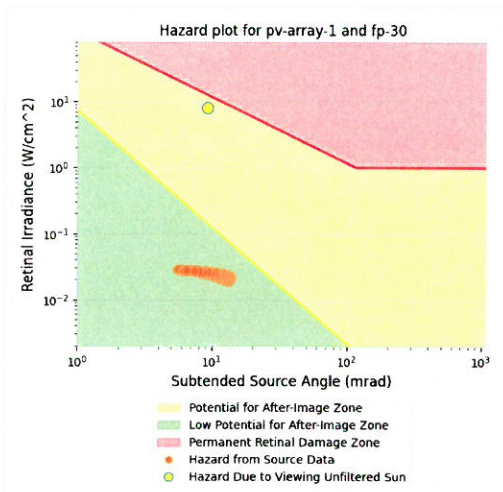
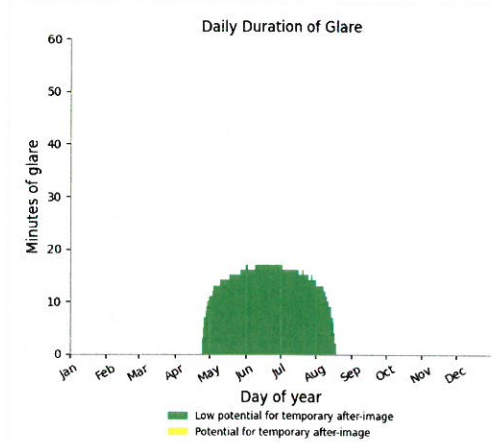
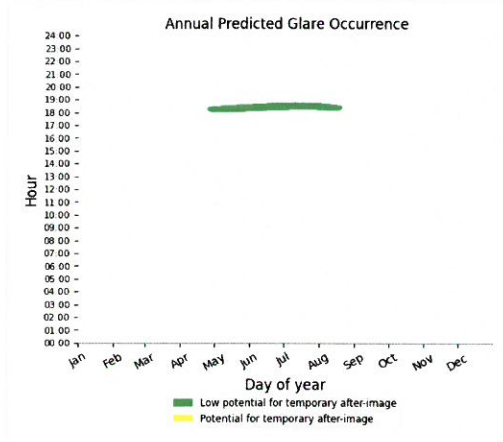
*No glare found*



### PV array 1 - Receptor (FP 30)

PV array is expected to produce the following glare for observers on this flight path:

- 1,641 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



**PV array 1 - Receptor (FP 32)**

*No glare found*

**PV array 1 - OP Receptor (1-ATCT)**

*No glare found*

**PV array 1 - Route Receptor (GA Rwy 14 Base)**

*No glare found*

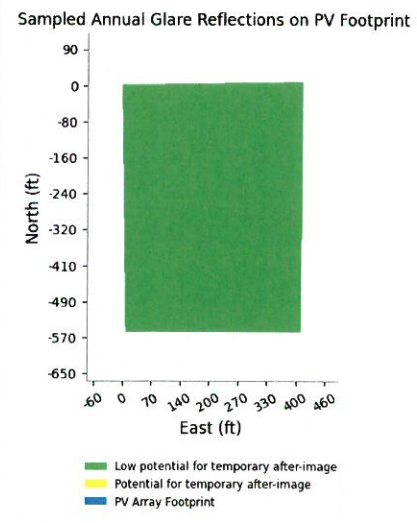
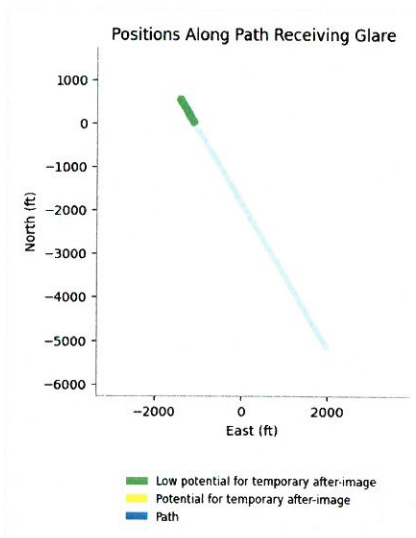
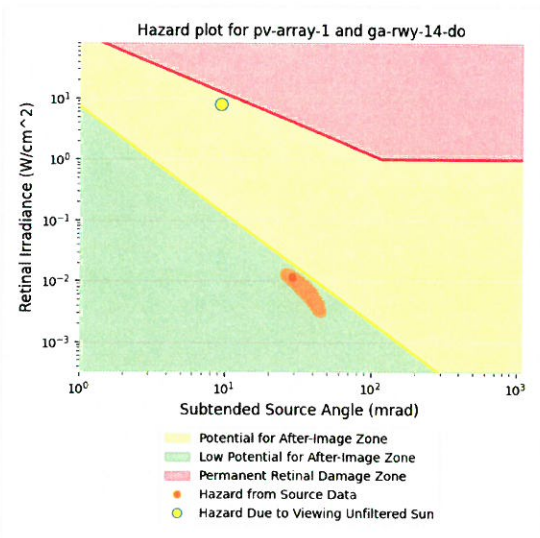
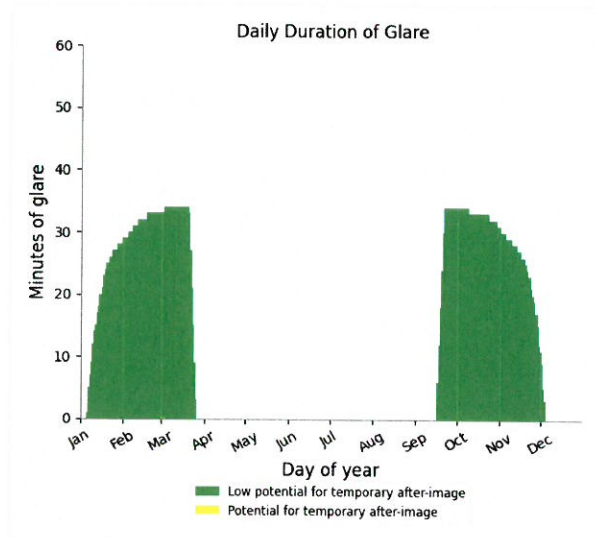
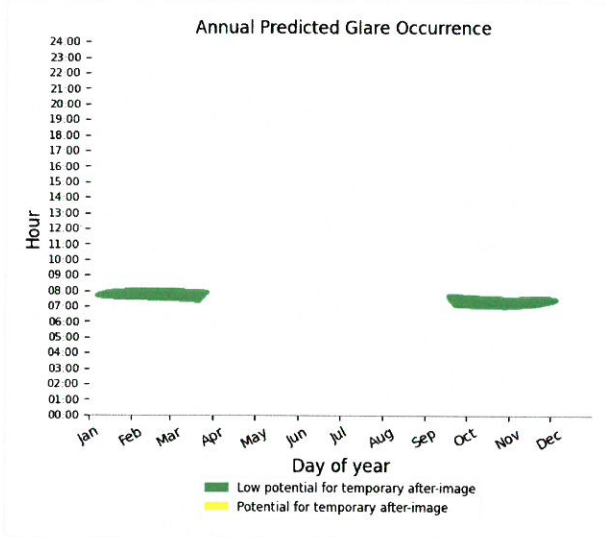
**PV array 1 - Route Receptor (GA Rwy 14 Crosswind )**

*No glare found*

### PV array 1 - Route Receptor (GA Rwy 14 Downwind)

PV array is expected to produce the following glare for receptors at this location:

- 4,399 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



**PV array 1 - Route Receptor (GA Rwy 14 Final)**

*No glare found*

**PV array 1 - Route Receptor (GA Rwy 14 Upwind)**

*No glare found*

**PV array 1 - Route Receptor (GA Rwy 32 Base)**

*No glare found*

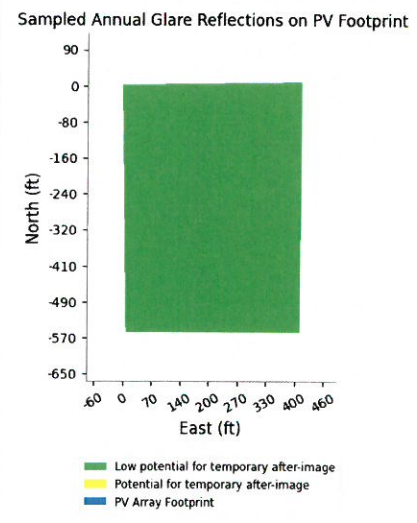
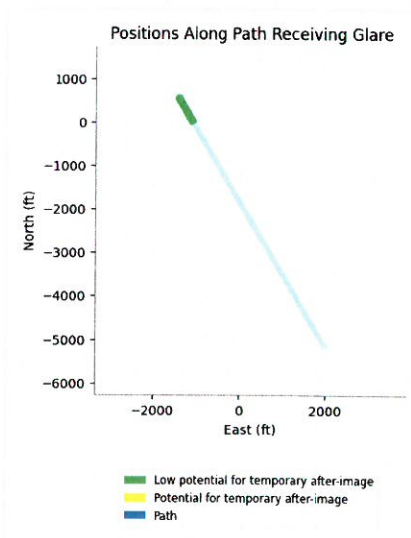
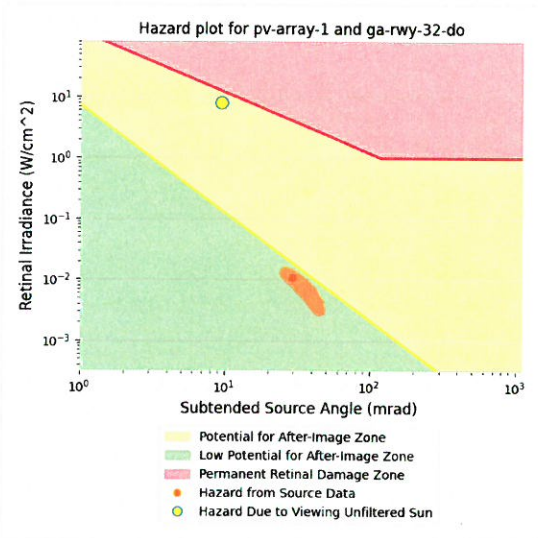
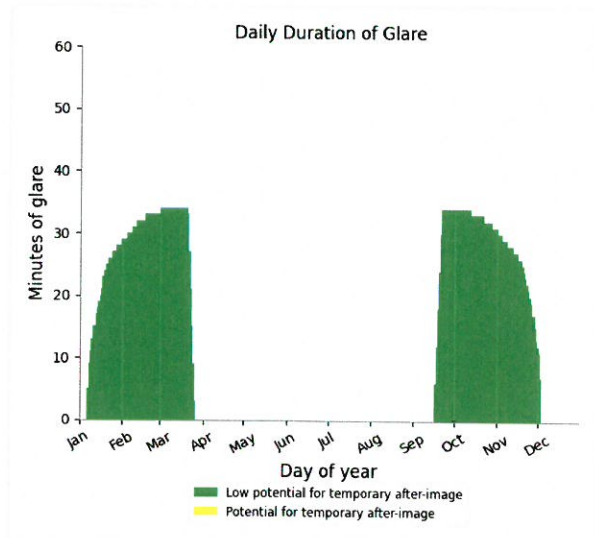
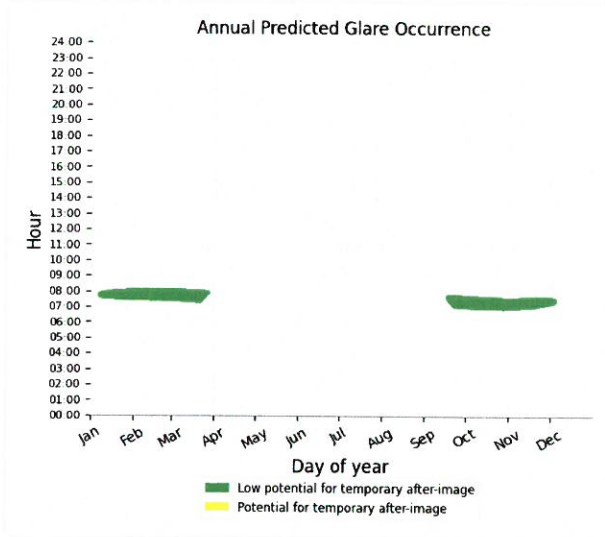
**PV array 1 - Route Receptor (GA Rwy 32 Crosswind)**

*No glare found*

### PV array 1 - Route Receptor (GA Rwy 32 Downwind)

PV array is expected to produce the following glare for receptors at this location:

- 4,402 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



## PV array 1 - Route Receptor (GA Rwy 32 Final)

*No glare found*

## PV array 1 - Route Receptor ( GA Rwy 32 Upwind )

*No glare found*

## Assumptions

- Times associated with glare are denoted in Standard time. For Daylight Savings, add one hour.
- Glare analyses do not account for physical obstructions between reflectors and receptors. This includes buildings, tree cover and geographic obstructions
- Detailed system geometry is not rigorously simulated.
- The glare hazard determination relies on several approximations including observer eye characteristics, angle of view, and typical blink response time. Actual values and results may vary.
- The system output calculation is a DNI-based approximation that assumes clear, sunny skies year-round. It should not be used in place of more rigorous modeling methods.
- Several V1 calculations utilize the PV array centroid, rather than the actual glare spot location, due to algorithm limitations. This may affect results for large PV footprints. Additional analyses of array sub-sections can provide additional information on expected glare.
- The subtended source angle (glare spot size) is constrained by the PV array footprint size. Partitioning large arrays into smaller sections will reduce the maximum potential subtended angle, potentially impacting results if actual glare spots are larger than the sub-array size. Additional analyses of the combined area of adjacent sub-arrays can provide more information on potential glare hazards. (See previous point on related limitations.)
- Hazard zone boundaries shown in the Glare Hazard plot are an approximation and visual aid. Actual ocular impact outcomes encompass a continuous, not discrete, spectrum.
- Glare locations displayed on receptor plots are approximate. Actual glare-spot locations may differ.
- Glare vector plots are simplified representations of analysis data. Actual glare emanations and results may differ.
- Refer to the **Help page** for detailed assumptions and limitations not listed here.

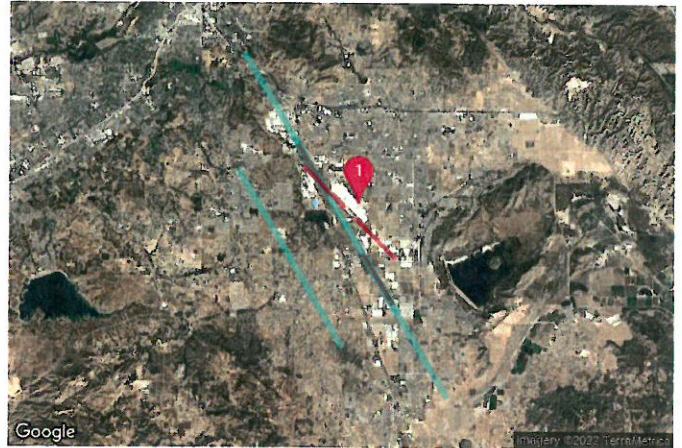


# Riverside CA 2

## Overhead Analysis

**Created** March 4, 2022  
**Updated** April 12, 2022  
**Time-step** 1 minute  
**Timezone offset** UTC-8  
**Site ID** 65668.11191

**Project type** Basic  
**Project status:** active  
**Category** 500 kW to 1 MW  
**(1,000 kW / 8 acre limit)**



### Misc. Analysis Settings

**DNI:** varies (1,000.0 W/m<sup>2</sup> peak)  
**Ocular transmission coefficient:** 0.5  
**Pupil diameter:** 0.002 m  
**Eye focal length:** 0.017 m  
**Sun subtended angle:** 9.3 mrad

### Analysis Methodologies:

- Observation point: **Version 2**
- 2-Mile Flight Path: **Version 2**
- Route: **Version 2**

## Summary of Results Glare with low potential for temporary after-image predicted

PV Name	Tilt	Orientation	"Green" Glare	"Yellow" Glare	Energy Produced
	deg	deg	min	min	kWh
PV array 1	10.0	194.0	10,223	0	-

# Component Data

## PV Array(s)

Total PV footprint area: 5.2 acres

**Name:** PV array 1  
**Footprint area:** 5.2 acres  
**Axis tracking:** Fixed (no rotation)  
**Tilt:** 10.0 deg  
**Orientation:** 194.0 deg  
**Rated power:** -  
**Panel material:** Smooth glass without AR coating  
**Vary reflectivity with sun position?** Yes  
**Correlate slope error with surface type?** Yes  
**Slope error:** 6.55 mrad

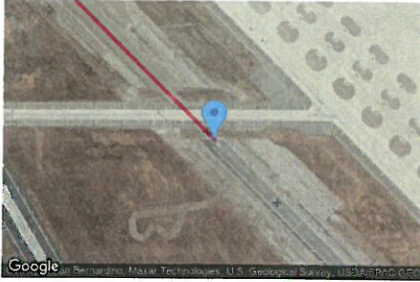
Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.892686	-117.279556	1560.93	0.00	1560.93
2	33.892704	-117.278215	1559.74	0.00	1559.74
3	33.891145	-117.278225	1558.97	0.00	1558.97
4	33.891145	-117.279523	1566.81	0.00	1566.81





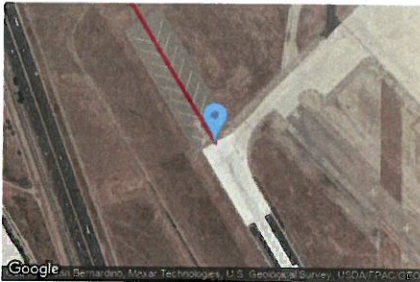
## 2-Mile Flight Path Receptor(s)

**Name:** FP 12  
**Description:**  
**Threshold height :** 50 ft  
**Direction:** 135.0 deg  
**Glide slope:** 3.0 deg  
**Pilot view restricted?** Yes  
**Vertical view restriction:** 30.0 deg  
**Azimuthal view restriction:** 50.0 deg



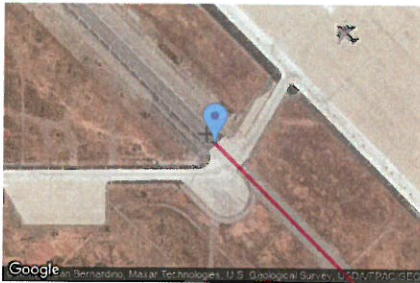
Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.890243	-117.260666	1519.07	50.00	1569.08
2-mile point	33.910687	-117.285323	1543.08	579.46	2122.53

**Name:** FP14  
**Description:**  
**Threshold height :** 56 ft  
**Direction:** 149.0 deg  
**Glide slope:** 2.59 deg  
**Pilot view restricted?** Yes  
**Vertical view restriction:** 30.0 deg  
**Azimuthal view restriction:** 50.0 deg



Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.896436	-117.270631	1536.71	56.00	1592.72
2-mile point	33.921216	-117.288597	1524.71	545.71	2070.42

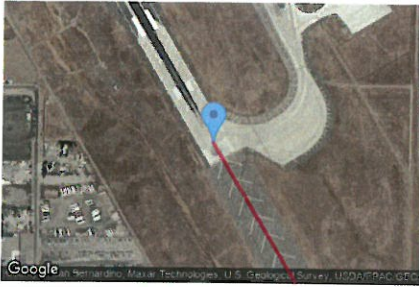
**Name:** FP 30  
**Description:**  
**Threshold height :** 50 ft  
**Direction:** 315.0 deg  
**Glide slope:** 3.0 deg  
**Pilot view restricted?** Yes  
**Vertical view restriction:** 30.0 deg  
**Azimuthal view restriction:** 50.0 deg



Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.884351	-117.253579	1507.14	50.00	1557.15
2-mile point	33.863907	-117.228924	1469.94	640.66	2110.60

Name: FP 32  
Description:  
Threshold height : 59 ft  
Direction: 329.0 deg  
Glide slope: 3.0 deg  
Pilot view restricted? Yes  
Vertical view restriction: 30.0 deg  
Azimuthal view restriction: 50.0 deg

Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.865319	-117.248518	1488.44	59.00	1547.45
2-mile point	33.840536	-117.230564	1460.06	640.84	2100.90



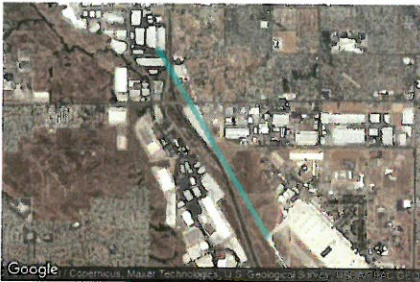
### Route Receptor(s)

**Name:** OHead Rwy 14 Downwind  
**Route type:** Two-way  
**View angle:** 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.863564	-117.293808	1500.14	2000.20	3500.34
2	33.908131	-117.325528	1500.14	2000.20	3500.34

**Name:** OHead Rwy 14 Final  
**Route type:** Two-way  
**View angle:** 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.925156	-117.291061	1500.14	2000.20	3500.34
2	33.896431	-117.270636	1500.14	0.00	1500.14

**Name:** OHead Rwy 14 Initial  
**Route type:** Two-way  
**View angle:** 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.968036	-117.322128	1500.14	2000.20	3500.34
2	33.880706	-117.259453	1500.14	2000.20	3500.34

**Name:** OHead Rwy 32 Downwind  
**Route type:** Two-way  
**View angle:** 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.863564	-117.293808	1500.14	2000.20	3500.34
2	33.819225	-117.262269	1500.14	2000.20	3500.34

Name: OHead Rwy 32 Final  
 Route type Two-way  
 View angle: 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.836269	-117.227869	1500.14	2000.20	3500.34
2	33.864994	-117.248281	1500.14	0.00	1500.14

Name: OHead Rwy 32 Initial  
 Route type Two-way  
 View angle: 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.793375	-117.196878	1500.14	2000.20	3500.34
2	33.880706	-117.259453	1500.14	2000.20	3500.34

### Discrete Observation Receptors

Number	Latitude	Longitude	Ground elevation	Height above ground	Total Elevation
	deg	deg	ft	ft	ft
1-ATCT	33.891568	-117.251182	1510.14	118.01	1628.15

1-ATCT map image



## Summary of PV Glare Analysis

*PV configuration and total predicted glare*

PV Name	Tilt	Orientation	"Green" Glare	"Yellow" Glare	Energy Produced	Data File
	deg	deg	min	min	kWh	
PV array 1	10.0	194.0	10,223	0	-	-

### Distinct glare per month

Excludes overlapping glare from PV array for multiple receptors at matching time(s)

PV	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec
pv-array-1 (green)	331	642	367	616	1860	1882	1903	1283	106	706	527	0
pv-array-1 (yellow)	0	0	0	0	0	0	0	0	0	0	0	0

## PV & Receptor Analysis Results

*Results for each PV array and receptor*

### PV array 1 low potential for temporary after-image

Component	Green glare (min)	Yellow glare (min)
FP: FP 12	0	0
FP: FP14	0	0
FP: FP 30	1641	0
FP: FP 32	0	0
OP: 1-ATCT	0	0
Route: OHead Rwy 14 Downwind	2679	0
Route: OHead Rwy 14 Final	0	0
Route: OHead Rwy 14 Initial	5903	0
Route: OHead Rwy 32 Downwind	0	0
Route: OHead Rwy 32 Final	0	0
Route: OHead Rwy 32 Initial	0	0

### PV array 1 - Receptor (FP 12)

*No glare found*

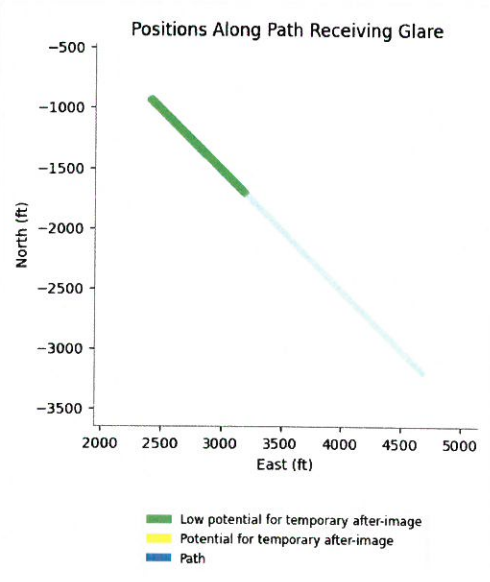
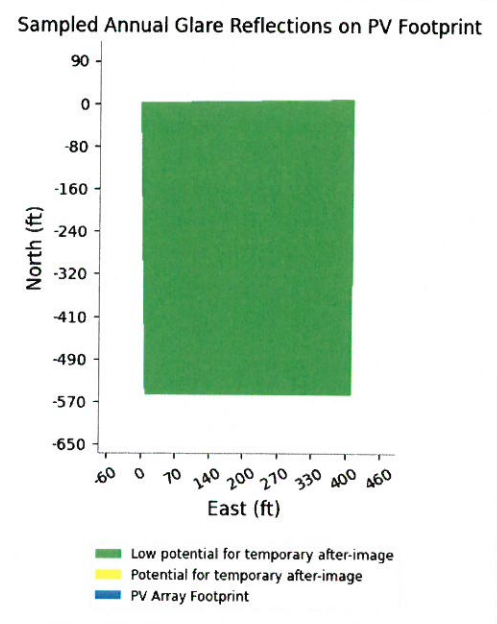
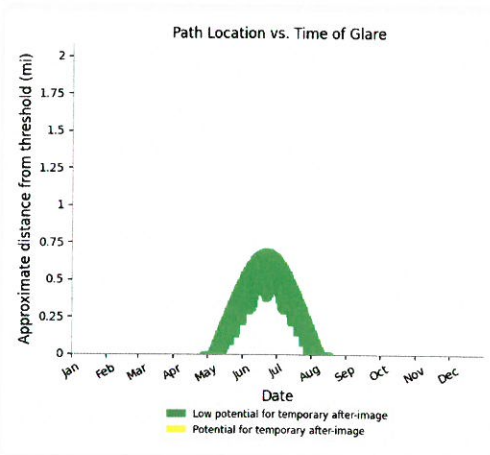
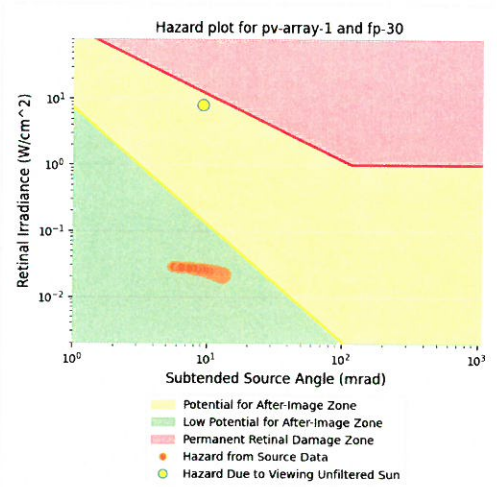
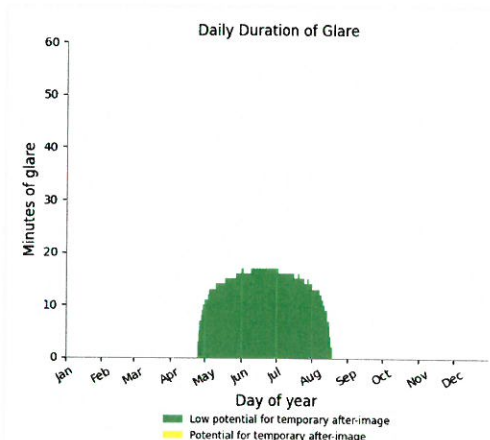
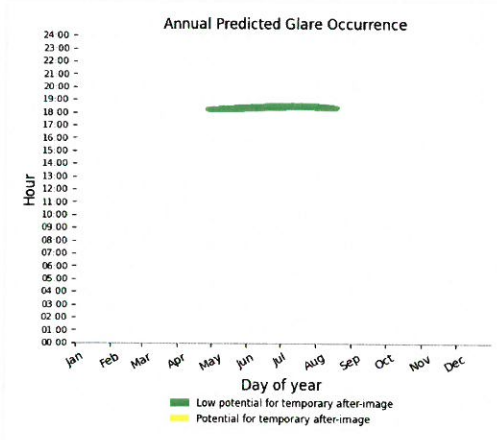
### PV array 1 - Receptor (FP14)

*No glare found*

### PV array 1 - Receptor (FP 30)

PV array is expected to produce the following glare for observers on this flight path:

- 1,641 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



**PV array 1 - Receptor (FP 32)**

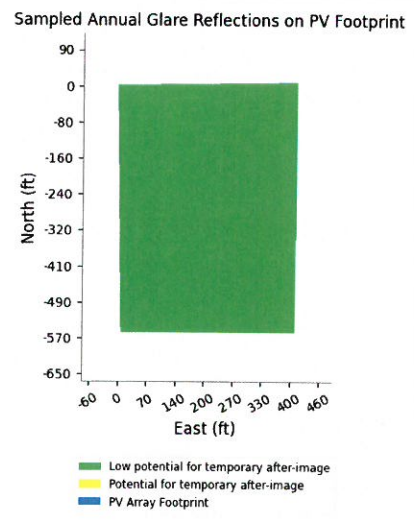
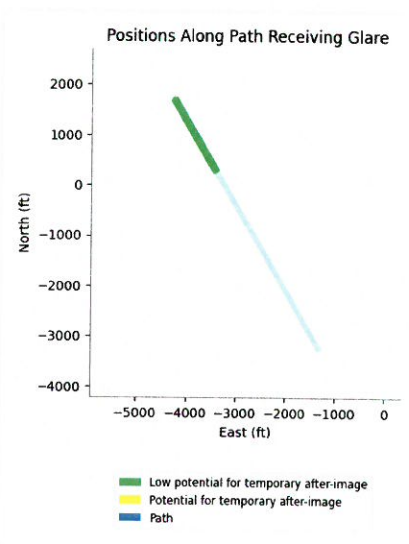
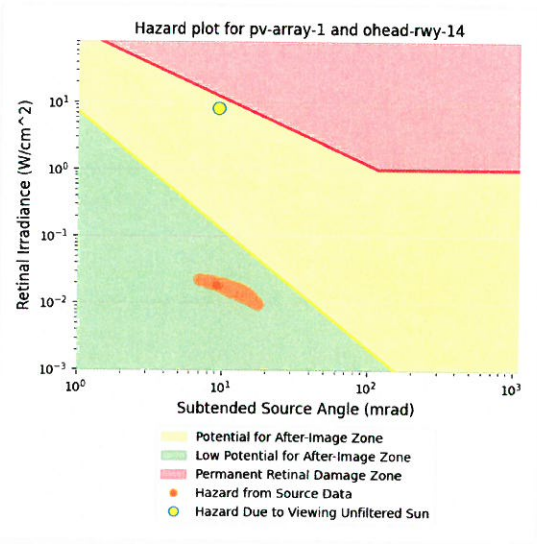
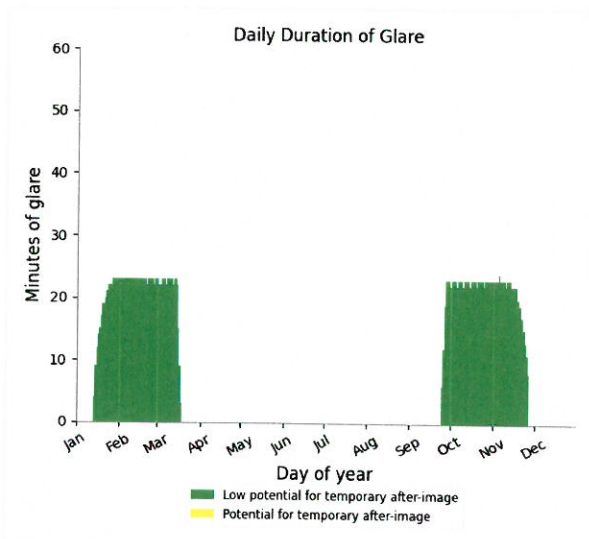
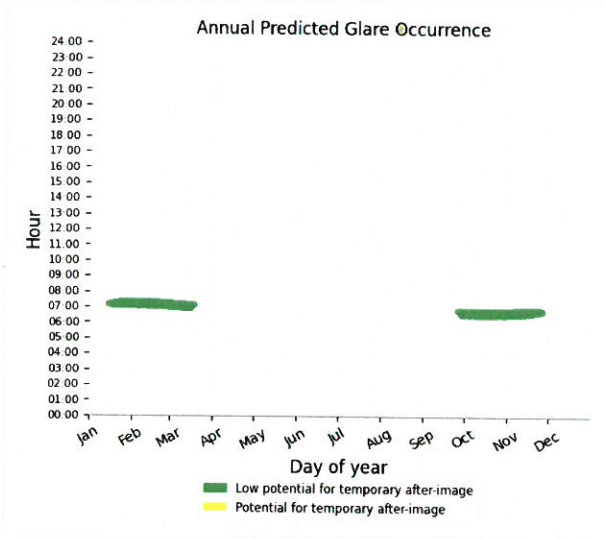
*No glare found*

**PV array 1 - OP Receptor (1-ATCT)**

*No glare found*

### PV array 1 - Route Receptor (OHead Rwy 14 Downwind )

- PV array is expected to produce the following glare for receptors at this location:
- 2,679 minutes of "green" glare with low potential to cause temporary after-image.
  - 0 minutes of "yellow" glare with potential to cause temporary after-image.



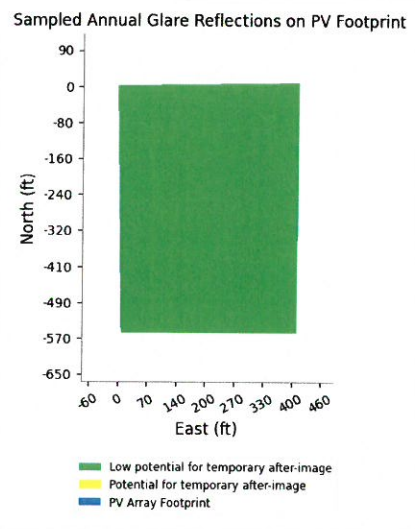
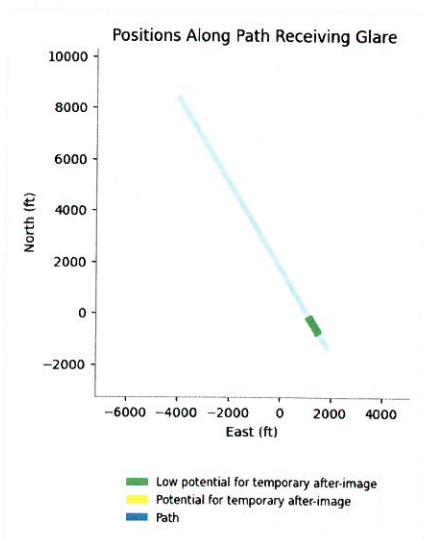
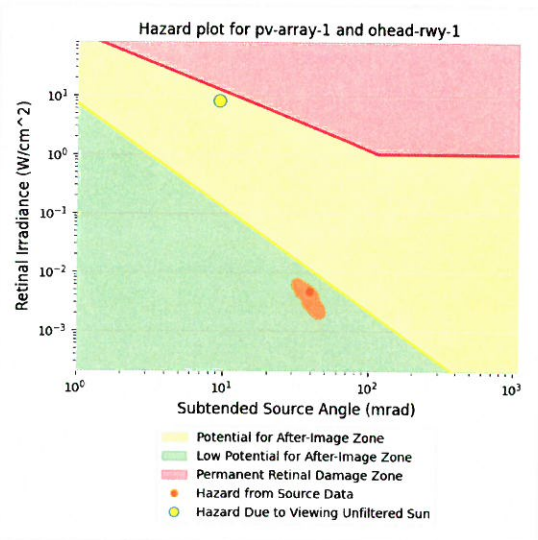
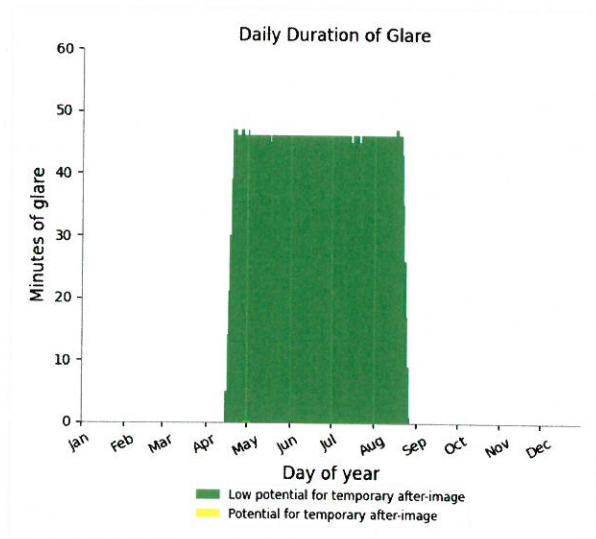
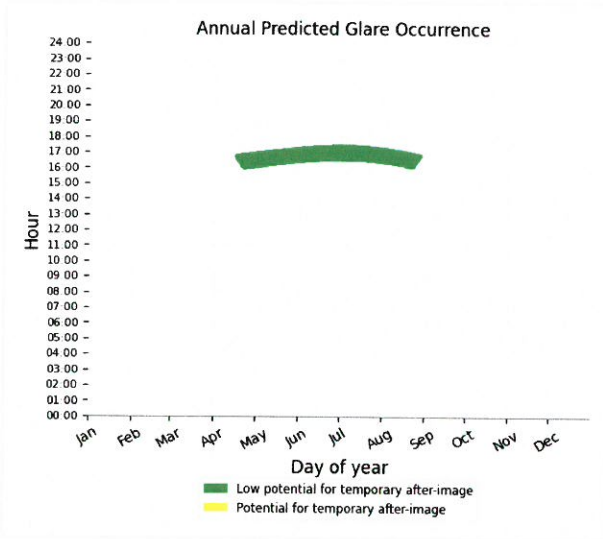


## **PV array 1 - Route Receptor (OHead Rwy 14 Final)**

*No glare found*

### PV array 1 - Route Receptor ( OHead Rwy 14 Initial)

- PV array is expected to produce the following glare for receptors at this location:
- 5,903 minutes of "green" glare with low potential to cause temporary after-image.
  - 0 minutes of "yellow" glare with potential to cause temporary after-image.



### **PV array 1 - Route Receptor (OHead Rwy 32 Downwind )**

*No glare found*

### **PV array 1 - Route Receptor (OHead Rwy 32 Final )**

*No glare found*

### **PV array 1 - Route Receptor ( OHead Rwy 32 Initial )**

*No glare found*

## **Assumptions**

- Times associated with glare are denoted in Standard time. For Daylight Savings, add one hour.
- Glare analyses do not account for physical obstructions between reflectors and receptors. This includes buildings, tree cover and geographic obstructions
- Detailed system geometry is not rigorously simulated.
- The glare hazard determination relies on several approximations including observer eye characteristics, angle of view, and typical blink response time. Actual values and results may vary.
- The system output calculation is a DNI-based approximation that assumes clear, sunny skies year-round. It should not be used in place of more rigorous modeling methods.
- Several V1 calculations utilize the PV array centroid, rather than the actual glare spot location, due to algorithm limitations. This may affect results for large PV footprints. Additional analyses of array sub-sections can provide additional information on expected glare.
- The subtended source angle (glare spot size) is constrained by the PV array footprint size. Partitioning large arrays into smaller sections will reduce the maximum potential subtended angle, potentially impacting results if actual glare spots are larger than the sub-array size. Additional analyses of the combined area of adjacent sub-arrays can provide more information on potential glare hazards. (See previous point on related limitations.)
- Hazard zone boundaries shown in the Glare Hazard plot are an approximation and visual aid. Actual ocular impact outcomes encompass a continuous, not discrete, spectrum.
- Glare locations displayed on receptor plots are approximate. Actual glare-spot locations may differ.
- Glare vector plots are simplified representations of analysis data. Actual glare emanations and results may differ.
- Refer to the **Help page** for detailed assumptions and limitations not listed here.

**NOTICE OF PUBLIC HEARING**  
**RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**  
[www.rcaluc.org](http://www.rcaluc.org)

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. **Information on how to participate in the hearing will be available on the ALUC website at [www.rcaluc.org](http://www.rcaluc.org).** The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893.**

The March Joint Powers Authority Planning Department should be contacted on non-ALUC issues. For more information, please contact March Joint Powers Authority Planner Mr. Jeffrey M. Smith at 951-656-7000.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website [www.rcaluc.org](http://www.rcaluc.org). Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to [prull@rivco.org](mailto:prull@rivco.org). Individuals with disabilities requiring reasonable modifications or accommodations, please contact Barbara Santos at (951) 955-5132.

**PLACE OF HEARING:** Riverside County Administration Center  
4080 Lemon Street, 1<sup>st</sup> Floor Board Chambers  
Riverside California

**DATE OF HEARING:** May 12, 2022

**TIME OF HEARING:** 9:30 A.M.

**CASE DESCRIPTION:**

ZAP1509MA22 – Black & Veatch – March Joint Powers Authority Case No. COM-Solar 22-003 (Building Permit). A proposal to construct an 84,000 square foot rooftop solar panel system on an existing industrial building on 45 acres, located northerly of Van Buren Boulevard, westerly of Opportunity Way, and easterly of Meridian Parkway (Airport Compatibility Zone B2 of the March Air Reserve Base/Inland Port Airport Influence Area).



# RIVERSIDE COUNTY

## AIRPORT LAND USE COMMISSION

### APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP1509MA22 DATE SUBMITTED: 2/10/2022

**APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION**

Applicant	<u>Scott Horsley</u>	Phone Number	<u>949-274-1011</u>
Mailing Address	<u>Blakc &amp; Veatch</u> <u>1721 Orchard Drive</u> <u>Newport Beach, CA 92660</u>	Email	<u>horsleys@bv.com</u>
Representative	<u>Same as Above</u>	Phone Number	_____
Mailing Address	_____	Email	_____
Property Owner	<u>Sysco Riverside Inc.</u>	Phone Number	<u>951-601-5300</u>
Mailing Address	<u>15750 Meridian Pkwy, Suite 140</u> <u>Riverside, CA 92518</u>	Email	_____

**LOCAL JURISDICTION AGENCY**

Local Agency Name	<u>MJPA</u>	Phone Number	<u>951-656-7000</u>
Staff Contact	<u>Jeffrey M. Smith</u>	Email	<u>smith@marchjp</u>
Mailing Address	<u>14205 Meridian Pwkwy, Suite</u> <u>Riverside, CA 92518</u>	Case Type	_____
Local Agency Project No	<u>COM-Solar22-003</u>	<input type="checkbox"/>	General Plan / Specific Plan Amendment
		<input type="checkbox"/>	Zoning Ordinance Amendment
		<input type="checkbox"/>	Subdivision Parcel Map / Tentative Tract
		<input type="checkbox"/>	Use Permit
		<input type="checkbox"/>	Site Plan Review/Plot Plan
		<input checked="" type="checkbox"/>	Other

**PROJECT LOCATION**

*Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways*

Street Address	<u>15750 Meridian Pkwy</u>		
Assessor's Parcel No.	<u>294-070-025</u>	Gross Parcel Size	<u>45 acres</u>
Subdivision Name	_____	Nearest Airport and distance from Airport	<u>March 1,500</u>
Lot Number	<u>9</u>		

**PROJECT DESCRIPTION**

*If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed*

Existing Land Use (describe)	<u>warehouse/ dry storage</u>
	<u>commercial food delivery services</u>

Proposed Land Use (describe)	No change, tenant improvemen to ad additional rooftop solar panels and ev charging		
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units) _____		
For Other Land Uses (See Appendix C)	Hours of Operation	8-5 M-F	
	Number of People on Site	100	Maximum Number _____
	Method of Calculation	_____	
Height Data	Site Elevation (above mean sea level)	1564	ft.
	Height of buildings or structures (from the ground)	50	ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	
	If yes, describe	Rooftop Solar Panels	

- A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME:** Estimated time for “staff level review” is approximately 30 days from date of submittal. Estimated time for “commission level review” is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:**
1. . . . . Completed ALUC Application Form
  1. . . . . ALUC fee payment
  1. . . . . Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
  1. . . . . Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
  1. . . . . CD with digital files of the plans (pdf)
  1. . . . . Vicinity Map (8.5x11)
  1. . . . . Detailed project description
  1. . . . . Local jurisdiction project transmittal
  3. . . . . Gummed address labels for applicant/representative/property owner/local jurisdiction planner
  3. . . . . Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. **(Only required if the project is scheduled for a public hearing Commission meeting)**

# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

## STAFF REPORT

**AGENDA ITEM:** 3.4

**HEARING DATE:** May 12, 2022

**CASE NUMBER:** ZAP1516MA22 – Optimus Building Corporation  
(Representative: Mike Naggar & Associates)

**APPROVING JURISDICTION:** City of Perris

**JURISDICTION CASE NO:** SPA22-05047 (Specific Plan Amendment), DPR22-00006  
(Development Plan Review)

**LAND USE PLAN:** 2014 March Air Reserve Base/Inland Port Airport Land Use  
Compatibility Plan

Airport Influence Area: March Air Reserve Base

Land Use Policy: Zones C1 and D

Noise Levels: Below 60 CNEL contour

**MAJOR ISSUES:** None

**RECOMMENDATION:** Staff recommends that the Commission find the proposed Specific Plan Amendment CONSISTENT with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, and also find the proposed Development Plan Review CONSISTENT, subject to the conditions included herein.

**PROJECT DESCRIPTION:** A proposal to construct an 878,750 square foot industrial manufacturing building with mezzanines on a 40.75 acres (approximately 4.5 acres of the site is planned for future commercial development – there are no entitlements proposed for this area at this time). The applicant also proposes amending the Perris Valley Commerce Center Specific Plan rezoning the site from Commercial to Light Industrial.

**PROJECT LOCATION:** The site is located northerly of Ramona Expressway, westerly of Redlands Avenue, easterly of Perris Boulevard, and southerly of Perry Street, approximately 9,184 feet southeasterly of the southerly end of Runway 14-32 at March Air Reserve Base.

### **BACKGROUND:**

Non-Residential Average Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zones C1 and D, where Zone C1 limits average intensity to 100 people per acre, and Zone D does not restrict non-residential intensity.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan and the Additional Compatibility Policies included in the March ALUCP, the following rates were used to calculate the occupancy for the proposed project:

- Manufacturing – 1 person per 200 square feet, and
- Office – 1 person per 200 square feet.

The project proposes to construct an 878,750 square foot industrial building with mezzanines, which includes 858,750 square feet of manufacturing area, 10,000 square feet of first floor office area, and 10,000 square feet of second floor office mezzanine area, accommodating a total occupancy of 4,394 people, resulting in an average intensity of 108 people per acre, which is not consistent with Zone C1 average intensity criterion of 100 people per acre, but consistent with Zone D as intensity is not restricted.

If we analyzed just the portion of the building located entirely within Zone C1 (as intensity is not restricted for the portion of the building located in Zone D), it would include 41,237 square feet of manufacturing area, 2,500 square feet of first floor office area, and 2,500 square feet of second floor office mezzanine area, accommodating an occupancy of 230 people, resulting in an average intensity of 56 people per acre, which is consistent with Zone C1 average intensity criterion of 100 people per acre.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle and 1.0 persons per trailer truck space). Based on the number of parking spaces provided (353 standard vehicles, 213 trailer spaces), the total occupancy would be estimated at 742 people for an average intensity of 18 people per acre, which is consistent with the Compatibility Zone C1 average intensity criterion of 100 people per acre, and Zone D where intensity is not restricted.

Non-Residential Single-Acre Intensity: Compatibility Zone C1 limits maximum single-acre intensity to 250 people, and Zone D does not restrict intensity. There are no risk-reduction design bonuses available, as March Air Reserve Base/Inland Port Airport is primarily utilized by large aircraft weighing more than 12,500 pounds.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre area (in Zone C1) would include 41,273 square feet of industrial manufacturing area, 2,500 square feet of first floor office area, and 2,500 square feet of second floor office mezzanine area, resulting in a single acre occupancy of 230 people which is consistent with the Compatibility Zone C1 single acre criterion of 250, zone D does not restrict non-residential intensity.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zones C1 and D (children's schools, day care centers, hospitals, nursing homes, libraries, places of assembly, highly noise-sensitive outdoor nonresidential uses and hazards to flight).

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being below the 60 CNEL range from aircraft noise. Therefore, no special measures are required to mitigate aircraft-generated noise.



Part 77: The elevation of Runway 14-32 at its southerly terminus is 1,488 feet above mean sea level (AMSL). At a distance of approximately 9,184 feet from the project to the nearest point on the runway, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,580 feet AMSL. The site's finished floor elevation is 1,459 feet AMSL and proposed building height is 50 feet, resulting in a top point elevation of 1,509 feet AMSL. Therefore, review of the building for height/elevation reasons by the FAA Obstruction Evaluation Service (FAAOES) is not required.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33C). The nearest portion of the project is located 9,184 feet from the runway, and therefore would be subject to the above requirement.

Although the nearest portion of the proposed project is located within 10,000 feet of the runway (approximately 8,551 feet), the project utilizes underground infiltration basins which will not contain surface water or attract wildlife and, therefore, would not constitute a hazard to flight.

Specific Plan Amendment: The applicant also proposes amending the Perris Valley Commerce Center Specific Plan rezoning the site from Commercial to Light Industrial. The proposed amendments would be consistent with the Compatibility Plan as long as the underlying development's intensity is consistent with the compatibility criteria.

**CONDITIONS:**

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight or circling climb following takeoff or toward an aircraft engaged in a straight or circling final approach toward a landing at an airport, other than a DoD or FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight or circling climb following takeoff or towards an aircraft engaged in a straight or circling final approach towards a landing at an airport.

- (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, congregate care facilities, places of assembly (including but not limited to places of worship and theaters)
  - (f) Highly noise-sensitive outdoor nonresidential uses. Examples of noise-sensitive outdoor nonresidential uses that are prohibited include, but are not limited to, major spectator-oriented sports stadiums, amphitheaters, concert halls and drive-in theaters.
  - (g) Other Hazards to flight.
3. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property, and be recorded as a deed notice.
4. The project has been conditioned to utilized underground detention systems, which shall not contain surface water or attract wildlife. Any other proposed basin would require review and approval by the ALUC. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at [RCALUC.ORG](http://RCALUC.ORG) which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

5. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
6. The project has been evaluated to construct an 878,750 square foot industrial building, which includes 858,750 square feet of industrial area, 10,000 square feet of first floor office area, and 10,000 square feet of second floor office mezzanine area. Any increase in building area, change in use to any higher intensity use, change in building location, or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.
7. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission and March Air Reserve Base.

X:\AIRPORT CASE FILES\March\ZAP1516MA22\ZAP1516MA22sr.doc

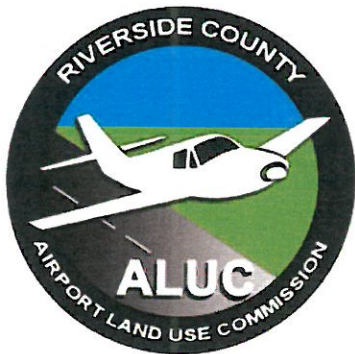
# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

# NOTICE

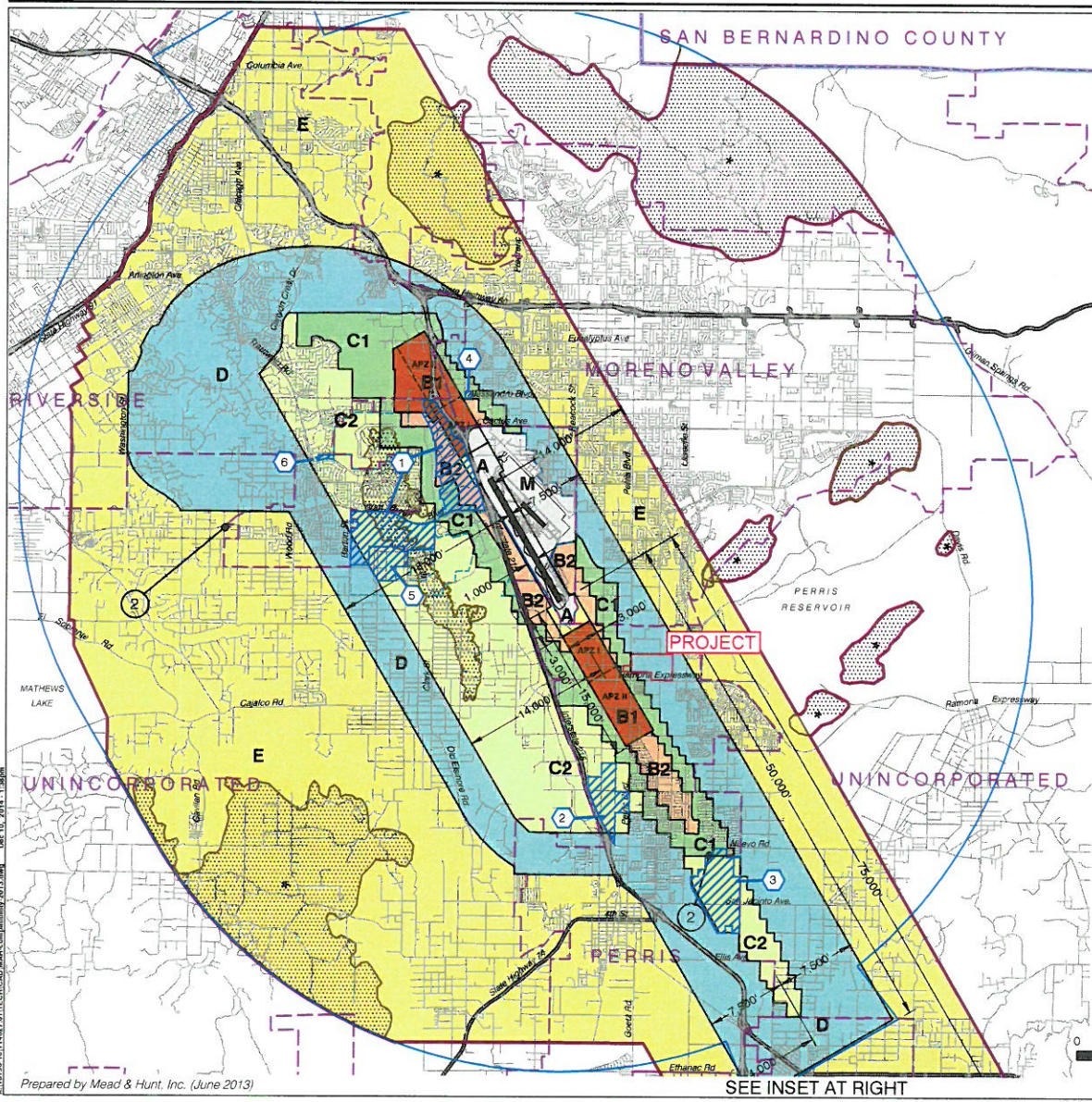
**THERE IS AN AIRPORT NEARBY.  
THIS STORM WATER BASIN IS DESIGNED TO HOLD  
STORM WATER FOR ONLY 48 HOURS AND  
NOT TO ATTRACT BIRDS**

**PROPER MAINTENANCE IS NECESSARY TO AVOID  
BIRD STRIKES**



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: \_\_\_\_\_ Phone: \_\_\_\_\_



**LEGEND**

**Compatibility Zones**

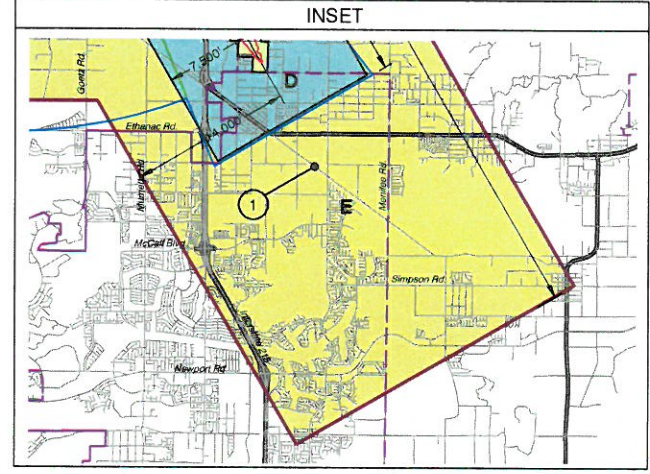
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

**Boundary Lines**

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

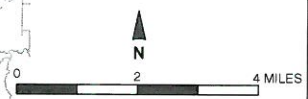
- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



**Riverside County  
Airport Land Use Commission  
March Air Reserve Base / Inland Port Airport  
Land Use Compatibility Plan  
(Adopted November 13, 2014)**

Note:  
All dimensions are measured from  
runway ends and centerlines.



Base map source: County of Riverside 2013

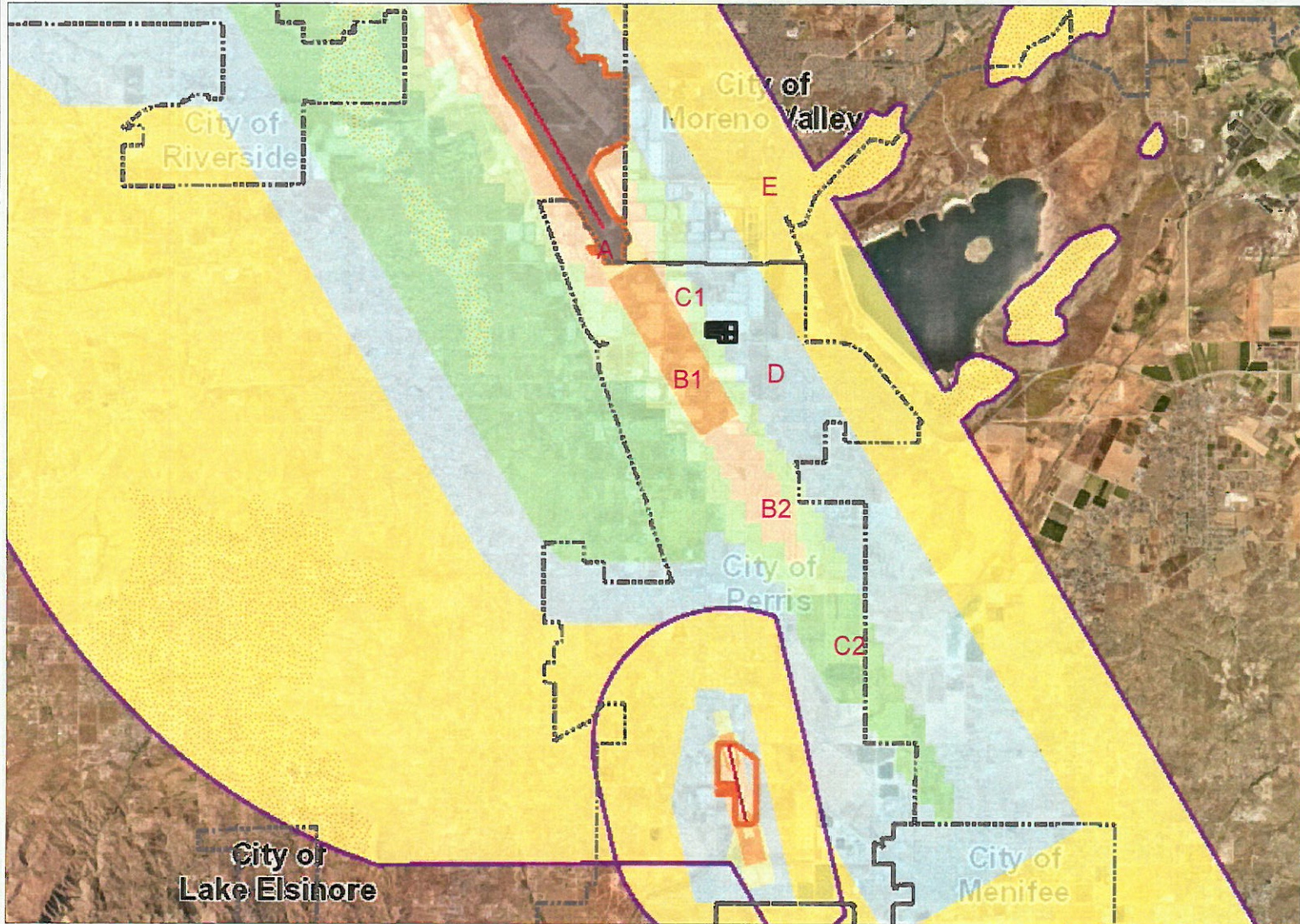
Map MA-1  
**Compatibility Map**  
March Air Reserve Base / Inland Port Airport

X:\BIBD\1011462\01\TECH\CAD\MAP\compatibility\_2013.dwg Dec 15, 2014 1:36pm

Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

# Map My County Map



## Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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## Notes



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# Map My County Map



**Legend**

- Runways
- Airports
- Airport Influence Areas

**Airport Compatibility Zones**

- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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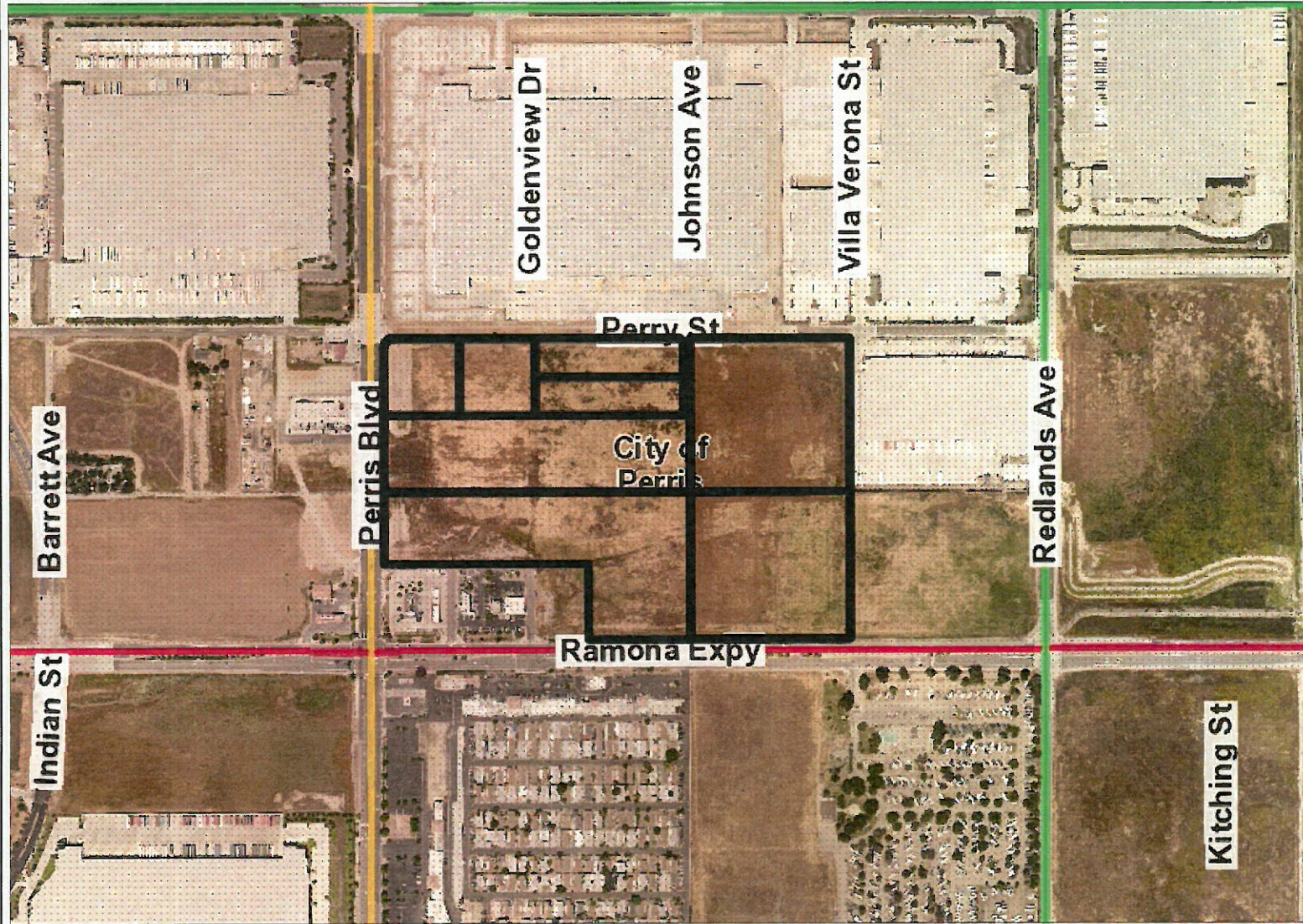
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**Notes**



# Map My County Map



- ### Legend
- Circulation Element**
    - ARTERIAL (128ft ROW)
    - COLLECTOR (74ft ROW)
    - EXPRESSWAY (184ft TO 220ft RC)
    - HIGHWAY (VARIABLE ROW)
    - INTERSTATE (VARIABLE ROW)
    - MAJOR (118ft ROW)
    - MOUNTAIN ARTERIAL (110ft ROV)
    - SECONDARY (100ft ROW)
    - URBAN ARTERIAL (152ft ROW)
  - County Centerline Names**
  - Blueline Streams**
  - City Areas**
  - World Street Map**



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### Notes

# Map My County Map



## Legend

- Blueline Streams
- ▤ City Areas
- World Street Map



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## Notes



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# Map My County Map



- Legend**
- Blueline Streams
  - ▨ City Areas
  - World Street Map



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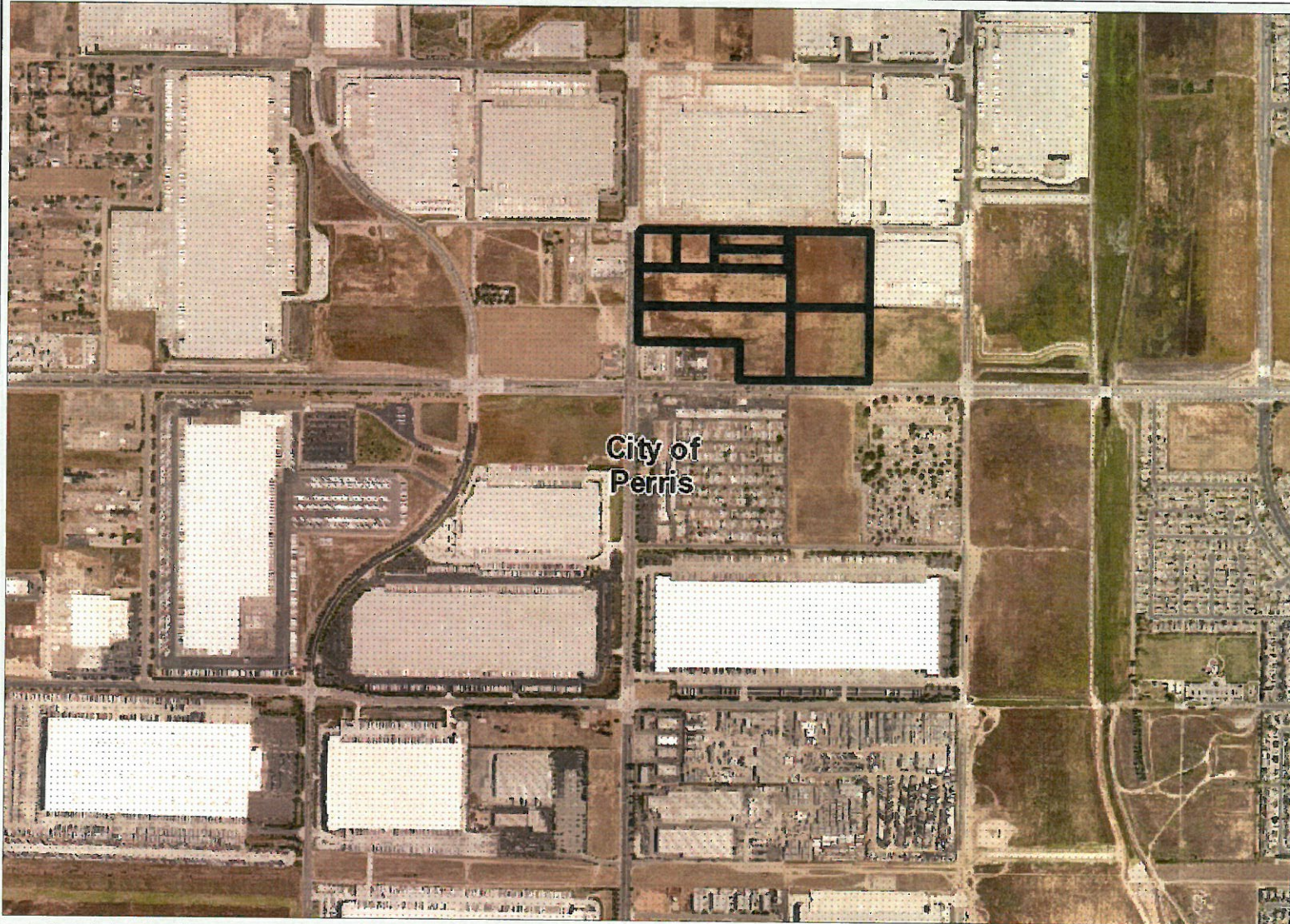
**Notes**



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# Map My County Map



## Legend

- County Centerline Names
- Blueline Streams
- City Areas
- World Street Map



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## Notes



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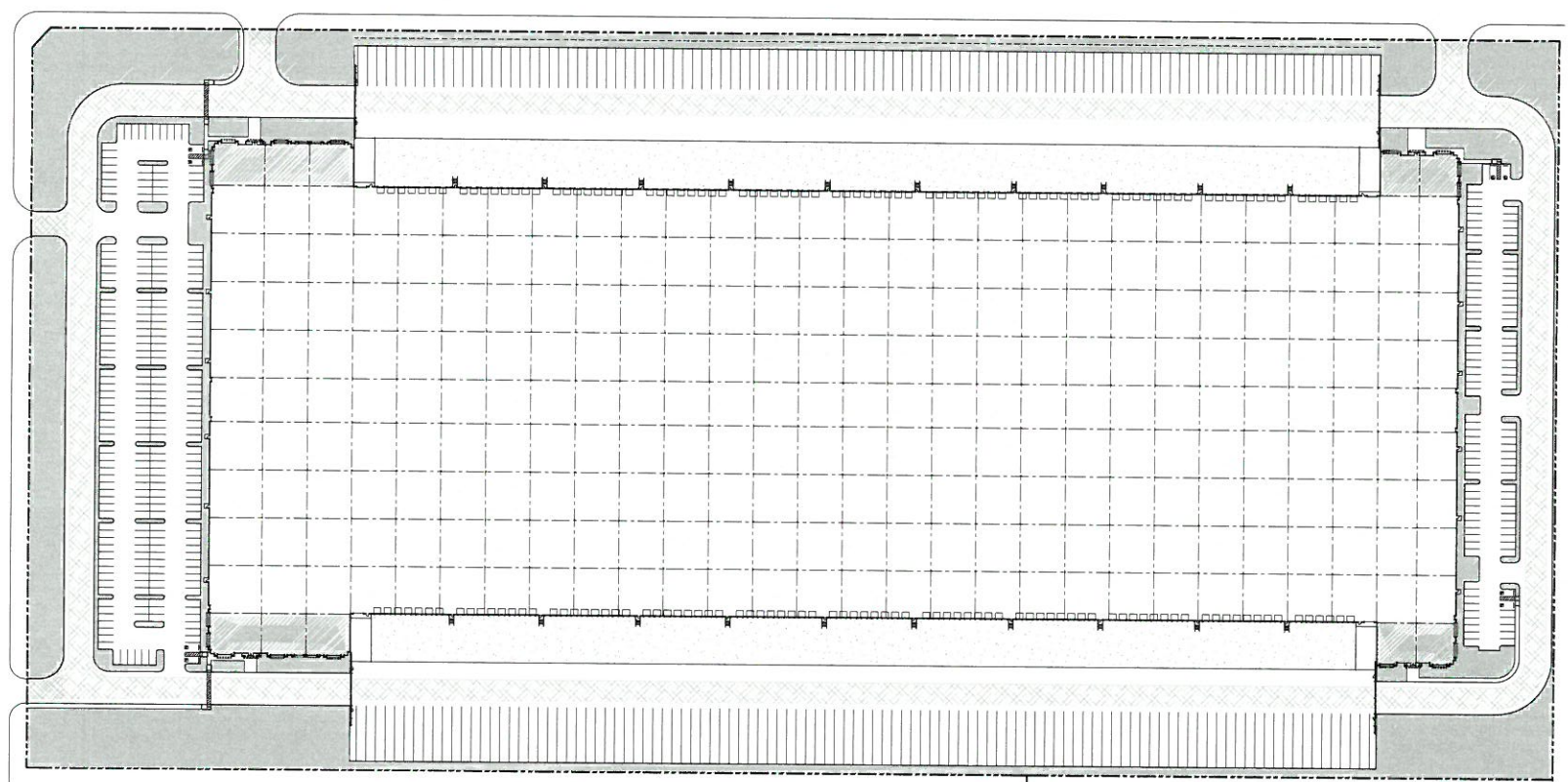
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**Owner:**  
OPTIMUS GROUP

RAMONA EXPY AND PERRIS BLVD,  
PERRIS, CALIFORNIA

**Project:**  
RAMONA EXPY. &  
PERRIS BLVD.  
PERRIS, CA

**Consultants:**



**OVERALL SITE PLAN**  
SCALE: 1" = 60'-0"  
NORTH

**PROPERTY OWNER**  
.....  
.....  
.....  
.....  
.....

**ADDRESS OF THE PROPERTY**  
.....  
.....  
.....

**ASSESSOR'S PARCEL NUMBER**  
.....  
.....  
.....

**LEGAL DESCRIPTION**  
.....  
.....  
.....  
.....  
.....

**ZONING**  
.....  
.....  
.....

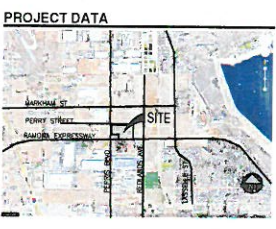
**APPLICANT**  
.....  
.....  
.....

**APPLICANT'S REPRESENTATIVE**  
.....  
.....  
.....

- SITE PLAN KEYNOTES**
- HEAVY BROOM FINISH CONCRETE PAVEMENT.
  - ASPHALT CONCRETE (AC) PAVING.
  - CONCRETE WALKWAY: MEDIUM BROOM FINISH.
  - ALTERNATIVE COLORED DRIVEWAY GRADING TO BE CONSTRUCTED.
  - 2"-45°-4" 6" THICK CONCRETE EXTERIOR LANDING AND TYP AT ALL EXTERIOR MAIN DOORS TO LANDSCAPED AREAS. FINISH TO BE MEDIUM BROOM FINISH.
  - PROVIDE 8" HIGH METAL GATES W/ ANDX-BOX PER FIRE DEPARTMENT STANDARDS FOR DRIVEWAY.
  - TRASH ENCLOSURE PER CITY STANDARD.
  - APPROXIMATE LOCATION OF TRANSFORMER.
  - FIRE-CAST CONCRETE WHEEL STOP.
  - CONCRETE FILLED GUARD POST "6 DIA UNO 42" H.
  - DESIGNATED SMOKING AREA.
  - LANDSCAPE: ALL LANDSCAPE AREAS INDICATED BY SHADING.
  - ACCESSIBLE ENTRY SIGN.
  - ACCESSIBLE PARKING STALL SIGN.
  - 8" HIGH CONCRETE RLT-UP SCREEN WALL.
  - 42" HIGH CONCRETE GUARDRAIL.
  - TRUNCATED DOME.
  - EMPLOYEE BREAK AREA.
  - EXTERIOR BIKE RACK.
  -

- SITE PLAN GENERAL NOTES**
- THE SITE PLAN BASED ON THE SOILS REPORT PREPARED BY TSD.
  - IF SOILS ARE EXPANSIVE IN NATURE, USE STEEL REINFORCING FOR ALL SITE CONCRETE.
  - ALL DIMENSIONS ARE TO THE FACE OF CONCRETE WALL, FACE OF CONCRETE CURB OR DRIP LINE UNLESS NOTED OTHERWISE.
  - SEE "C" PLANS FOR ALL CONCRETE CURBS, CUTTERS AND GABLES.
  - THE ENTIRE PROJECT SHALL BE PERMANENTLY MAINTAINED WITH AN AUTOMATIC IRRIGATION SYSTEM.
  - SEE "C" DRAWINGS FOR POINT OF CONNECTIONS TO OFF-SITE UTILITIES. CONTRACTOR SHALL VERIFY ACTUAL UTILITY LOCATIONS.
  - PROVIDE POSITIVE DRAINAGE AWAY FROM BLDG. SEE "C" DRAWINGS.
  - CONTRACTOR TO REFER TO "C" DRAWINGS FOR ALL HORIZONTAL CONTROL DIMENSIONS. SITE PLANS ARE FOR GUIDANCE AND STARTING LAYOUT POINTS.
  - SEE "C" DRAWINGS FOR FINISH GRADE ELEVATIONS.
  - CONCRETE SEPARATORS TO BE A MINIMUM OF 4" THICK W/ TOLDED JOINTS AT 8' O.C. EXPANSION/CONSTRUCTION JOINTS SHALL BE A MAXIMUM 12' EA. MAX. EXPANSION JOINTS TO HAVE COMPRESSIVE EXPANSION FILLER MATERIAL OF 1/4" FINISH TO BE A MEDIUM BROOM FINISH UNLESS NOTED OTHERWISE.
  - PAINT CURBS AND PROVIDE SIGNS TO INFORM OF FIRE LANES AS REQUIRED BY FIRE DEPARTMENT.
  - CONSTRUCTION DOCUMENTS PERTAINING TO THE LANDSCAPE AND IRRIGATION OF THE ENTIRE PROJECT SITE SHALL BE SUBMITTED TO THE BUILDING DEPARTMENT AND APPROVED BY PUBLIC FACILITIES DEVELOPMENT PRIOR TO COMMENCEMENT OF CONSTRUCTION.
  - IN ADDITION TO FINAL CITY INSPECTION, THE LANDSCAPE ARCHITECT SHALL SUBMIT A CERTIFICATE OF COMPLETION TO PUBLIC FACILITIES DEVELOPMENT.
  - ALL LANDSCAPE AND IRRIGATION DESIGNS SHALL MEET CURRENT CITY STANDARDS AS LISTED IN ORDINANCES OR AS OBTAINED FROM PUBLIC FACILITIES DEVELOPMENT.
  - LANDSCAPED AREAS SHALL BE DELINEATED WITH A MINIMUM SIX INCHES (6") HIGH CURB.
  - ALL GROUND MOUNTED UTILITY STRUCTURES SUCH AS TRANSFORMERS, MANHOLE COVERS AND SIGN FOUNDATION WALLS SHALL BE LOCATED OUT OF VIEW FROM A PUBLIC STREET OR ADEQUATELY SCREENED THROUGH THE USE OF LANDSCAPING AND/OR WOODEN WALLS.
  - ALL LIGHTING FIXTURES TO BE FULLY SHIELDED WITH CUT-OFF FIXTURES THAT EMITS GLARE ONTO ADJACENT PROPERTIES.
  - PARKING AREA LIGHTING TO BE PROVIDED PURSUANT TO SECTION 19.02.110.9 OF PACC SPECIFIC PLAN.

- SITE LEGEND**
- CONCRETE PAVING SEE "C" DRAWINGS FOR THICKNESS
  - ASPHALT CONCRETE PAVING SEE "C" DRAWINGS FOR THICKNESS
  - STANDARD PARKING STALL (9' x 19')
  - DISABLED PARKING STALL (9' x 19') + 5' W/ ACCESSIBLE AISLE
  - DISABLED PARKING (VAN) STALL (12' x 18') + 5' W/ ACCESSIBLE AISLE
  - LANDSCAPED AREA
  - PATH OF TRAVEL
  - 20' FIRE WIDE FREELANE
  - EXISTING PUBLIC FIRE HYDRANT
  - PRIVATE FIRE HYDRANT - APPROXIMATE LOCATION



**VICINITY MAP**

SITE AREA	
714 s.f.	1,775,193 s.f.
In acres	40.75 ac
BUILDING AREA	
Office - 1st floor	10,000 s.f.
Office - 2nd floor	10,000 s.f.
Warehouses	855,750 s.f.
<b>TOTAL</b>	<b>878,750 s.f.</b>
	<b>49.5%</b>
COVERAGE	
<b>AUTO PARKING REQUIRED</b>	
High Cube	nil
Office: 1,300 s.f. (if exceeds 10% of GFA)	20 stalls
Warehouse: 1st ZON @ 1/1,000 s.f.	10 stalls
2nd ZON @ 1/2,000 s.f.	188 stalls
above 40K @ 1/5,000 s.f.	196 stalls
<b>Total parking requires</b>	<b>196 stalls</b>
<b>AUTO PARKING PROVIDED</b>	
Stalls (9' x 19')	354 stalls
<b>TRAILER PARKING PROVIDED</b>	
Trailer (12' x 45')	213 stalls
ZONING PERFORMANCE FOR CITY	
Zoning: Over-Height - Light Industrial (LI)	
<b>MAXIMUM BUILDING HEIGHT ALLOWED</b>	
Height - 50'	
<b>MAXIMUM BUILDING COVERAGE</b>	
Coverage - 50%	
<b>LANDSCAPE REQUIREMENT</b>	
Percentage - 12%	
<b>LANDSCAPE PROVIDED</b>	
Percentage	12.1%
s.f.	215,900 s.f.
RETRIBUTES	
Penny street - 10'	
Ramona Expy - 20'	
Perris Blvd - 20'	
Solar/Water - None	

**OFFICIAL USE ONLY**

IF THIS SHEET IS NOT A 30" X 42" IT IS A REDUCED PRINT

Title: OVERALL SITE PLAN

Project Number: 20555

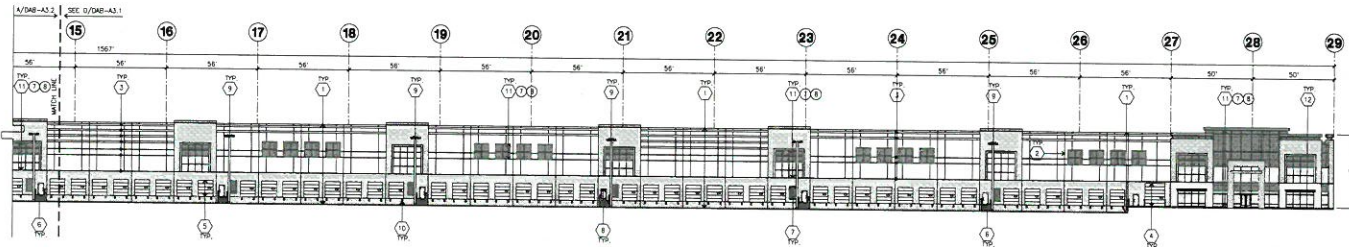
Drawn by: RC

Date: -

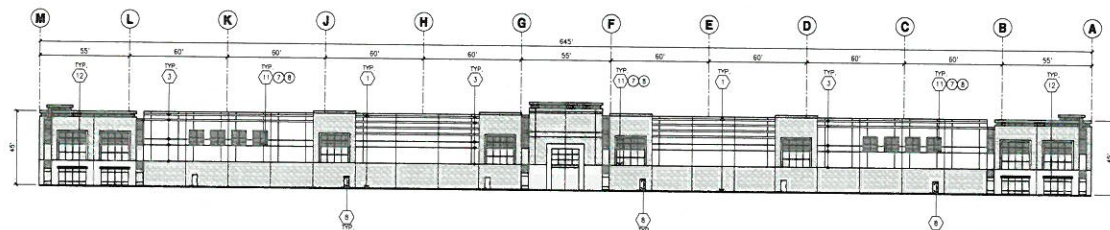
Revision: -

Sheet:

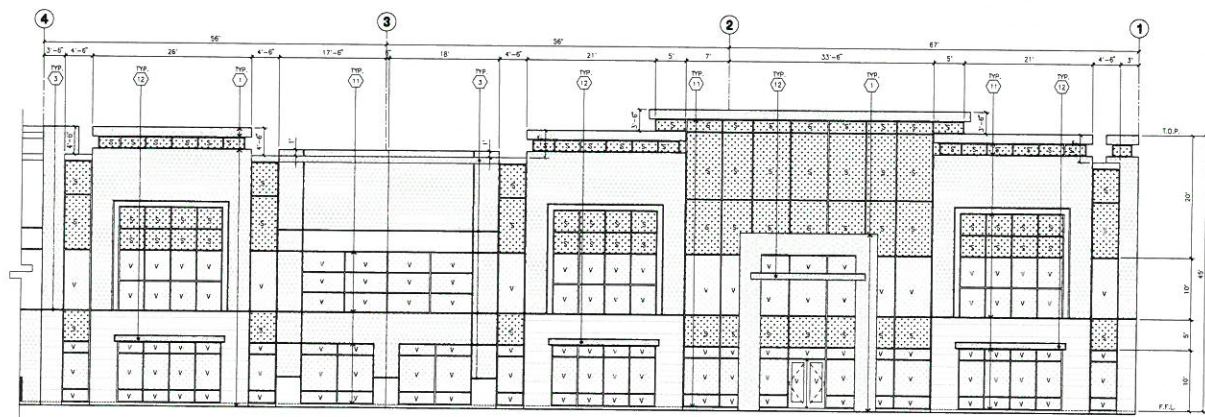
**DAB-A1.1**



**SOUTH ELEVATION CONT.**  
SCALE: 1/8" = 1'-0"



**EAST ELEVATION**  
SCALE: 1/8" = 1'-0"



**ENLARGED NORTH ELEVATION**  
SCALE: 1/8" = 1'-0"

**ELEVATION KEYNOTES**

1. CONCRETE FLT-UP PANEL
2. PANEL JOINT
3. PANEL REVEAL - ALL REVEALS TO HAVE A MAX. OF 1/8" CHAMFER
4. REVEAL COLOR TO MATCH ADJACENT BUILDING FLEED COLOR UNLESS NOTED OTHERWISE
5. IF A 10" OVERHEAD DOOR IS DRIVE THRU, PROVIDE COMPLETE WEATHER-STRIPPING PROTECTION ALL AROUND
6. CONCRETE STAIR LANDING AND CORRIORAIL W/ METAL PIPE HANDRAIL
7. PROVIDE NON-SKID TREADING TO MEET ADA REQUIREMENTS. PROVIDE CONTRASTING COLORED 3" WIDE WARNING STRIPS INTERNAL TO CONCRETE AT TOP LANDING AND BOTTOM TREAD FOR ADA REQUIREMENTS
8. IF METAL COVER OPENING FOR VENTILATION, PAINT TO MATCH BUILDING FLEED COLOR
9. METAL DOORS - PROVIDE COMPLETE WEATHER STRIPING ALL AROUND DOOR
10. EXTERIOR DOWN SPOUTS W/ 2 OVERFLOW SCUPPERS
11. DOOR DOOR SPOUTER TYPICAL
12. ALUMINUM EXTERIOR FINISHING WITH TEMPERED GLAZING AT ALL DOORS. SPOUTLES ADJACENT TO DOORS AND GLAZING WITH BOTTOMS LESS THAN 18" ABOVE FINISH FLOOR ELEVATION
13. METAL CANOPY
14. KNOCK OUT PANEL
15. INTERIOR ROOF DRAIN W/ 2 OVERFLOW SCUPPERS
16. CONC. FILLED GUARD POST 6" DIA. UNO. 42"H.

**ELEVATION GENERAL NOTES**

1. ALL PAINT COLOR CHANGES TO OCCUR AT INSIDE CORNERS UNLESS NOTED OTHERWISE
2. ALL PAINT FINISHES ARE TO BE FLAT UNLESS NOTED OTHERWISE
3. T.O.P. EL. = TOP OF PARAPET ELEVATION
4. F.F. = FINISH FLOOR ELEVATION
5. STOREFRONT CONSTRUCTION: GLASS, METAL ATTACHMENTS AND LINTELS SHALL BE DESIGNED TO RESIST 15 MPH EXPOSURE. IC FINISH CONTRACTOR SHALL SUBMIT SHOP DRAWINGS PRIOR TO INSTALLATION
6. CONTRACTOR SHALL FULLY PAINT ONE CONCRETE PANEL W/ SELECTED COLOR. ARCHITECT AND OWNER SHALL APPROVE PRIOR TO PAINTING REMAINDER OF BUILDING
7. BACK SIZE OF PARAPETS TO HAVE SMOOTH FINISH AND BE PAINTED WITH EASTASTONING PAINT
8. FOR SPANDREL GLAZING, ALLOW SPACE BEHIND SPANDREL TO BREATHE
9. USE ADHESIVE BACK WOOD STRIPS FOR ALL REVEAL FORMS
10. THE FIRST COAT OF PAINT TO BE ROLLED-ON AND THE SECOND COAT TO BE SPRAYED-ON

**ELEVATION COLOR LEGEND/SCHED.**

1. CONCRETE FLT-UP PANEL COLOR - SHERWIN WILLIAMS SW 7003 PURE WHITE
2. CONCRETE FLT-UP PANEL COLOR - SHERWIN WILLIAMS SW 7021 GRAY SCREEN
3. CONCRETE FLT-UP PANEL COLOR - SHERWIN WILLIAMS SW 7022 ONLINE
4. CONCRETE FLT-UP PANEL COLOR - SHERWIN WILLIAMS SW 7023 NETWORK GRAY
5. CONCRETE FLT-UP PANEL COLOR - SHERWIN WILLIAMS SW 7024 SOFTWARE
6. CONCRETE FLT-UP PANEL COLOR - SHERWIN WILLIAMS SW 7025 WEB GRAY
7. GLAZING COLOR - BLUE REFLECTIVE GLAZING
8. MULLIONS COLOR - BLACK ANODIZED
9. METAL CANOPY - SHERWIN WILLIAMS PRO-INDUSTRIAL WATER-BASED (BS3-1150) SEAP-GLOSS IN COLOR SW 7075 WEB GRAY
10. DOOR COLORS - SHERWIN WILLIAMS SW 7003 PURE WHITE
11. CONCRETE FLT-UP PANEL - 1/4" IPS SIDING (BEASED-EDGE) LONGEST LENGTH IS 20' MINIMUM JOINTS MINIMUM STACKING

**GLAZING LEGEND**

- NOTE: ALL EXTERIOR AND INTERIOR GLAZING SHALL BE TEMPERED
- SPANDREL GLASS
  - VISION GLASS
- ALL GLAZING TO BE TEMPERED
- GLAZING: PPG SOLARCOOL (2) GRANITE II 4 SOLARBAN 60 (3) CLEAR  
 (1) 3/32" SPACED GL. VET. DR.  
 (2) INSULATED GLASS UNIT WITH 1/2" AIRSPACE AND (2) 1/4" LITES
- SPANDREL: 1/4" SOLARCOOL GRANITE SPANDREL W/ HARMONY GRAY SPACED GL. PRINTED ON REFLECTIVE
- MULLIONS: ANODIZED CLEAR

**HPA**  
architecture

7031 P.C.  
16831 BARBARAN AVENUE - STE.  
#100 IRVINE CA  
92612  
HE 949-854-1770  
FAX 949-854-0851  
e-mail hpa@hpaarch.com

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Owner:  
**OPTIMUS GROUP**

---

Project:  
**RAMONA EXPY.  
&  
PERRIS BLVD.**

PERRIS, CA

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Consultants:

---

Title: **ELEVATIONS**

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Project Number: 20505  
Drawn by: RC  
Date: -  
Revision:

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Sheet  
**DAB-A3.2**

CAUTION: IF THIS SHEET IS NOT A 30" X 42" IT IS A REDUCED PRINT

**ELEVATION KEYNOTES**

1. CONCRETE TILT-UP PANEL.
2. PANEL JOINT.
3. REVEAL DOOR TO MATCH ADJACENT BUILDING FLOOR COLOR. 1/2" O.D.
4. 12" X 14" OVERHEAD DOOR @ DRIVE THRU. PROVIDE COMPLETE WEATHER-STRIPPING PROTECTION ALL AROUND.
5. 8" X 10" OVERHEAD DOOR @ DRIVE THRU. PROVIDE COMPLETE WEATHER-STRIPPING PROTECTION ALL AROUND.
6. CONCRETE STAIR LANDING AND GUARDRAIL W/ METAL PIPE HANDRAIL. PROVIDE NON SLIP TREADING TO MEET ADA REQUIREMENTS. PROVIDE CONTRASTING COLORED 2" WIDE WARNING STRIPES INTEGRAL TO CONCRETE AT TOP LANDING AND BOTTOM TREAD FOR ADA REQUIREMENTS.
7. 4" X 8" METAL LOUVER OPENING FOR VENTILATION. PAINT TO MATCH BUILDING COLOR.
8. METAL DOORS. PROVIDE COMPLETE WEATHER STRIPPING ALL AROUND DOOR.
9. EXTERIOR DOWN SPOUTS W/ 2 OVERFLOW SCUPPERS.
10. DOOR DOOR BUMPER TYPICAL.
11. ALUMINUM STOREFRONT FRAMING WITH TEMPERED GLAZING AT ALL DOORS. LOCATES ADJACENT TO DOORS AND GLAZING WITH BOTTOMS LESS THAN 18" ABOVE FINISH FLOOR ELEVATION.
12. METAL CANOPY.
13. KNOCK OUT PANEL.
14. INTERIOR ROOF DRAIN W/ 2 OVERFLOW SCUPPERS.
15. CONC. FILLED GUARD POST. 6" DIA. U.N.O. 42".



**ELEVATION GENERAL NOTES**

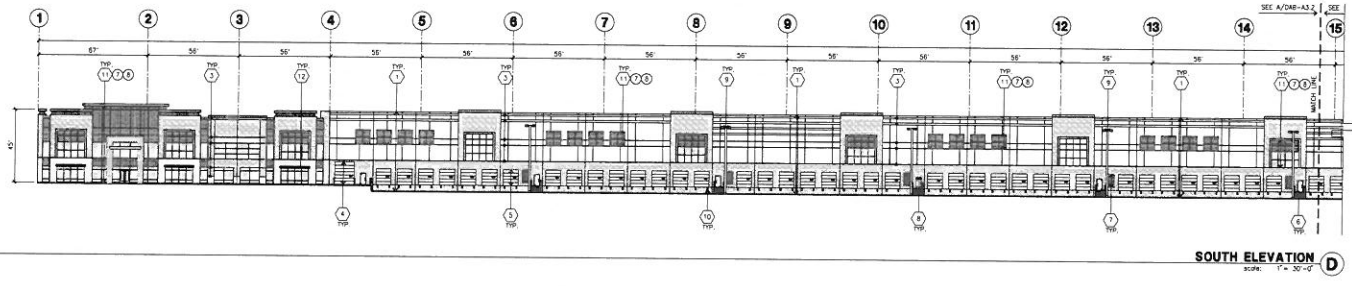
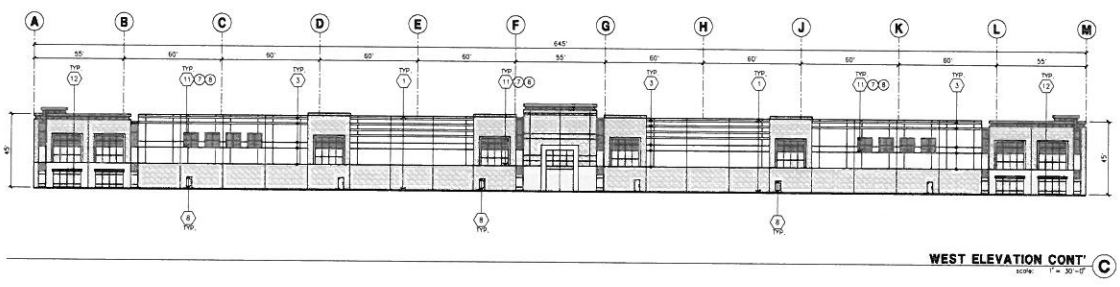
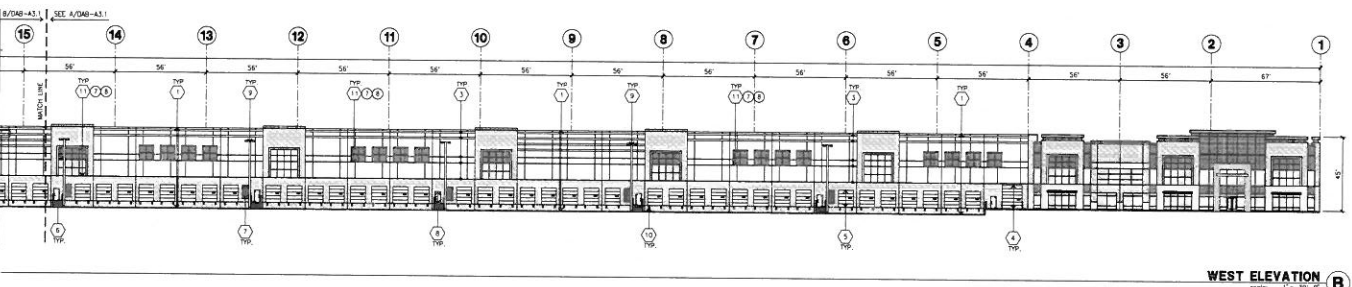
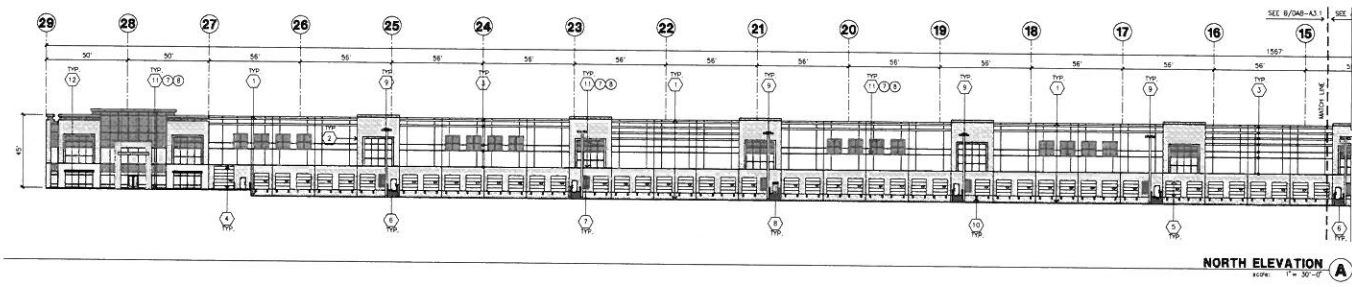
1. ALL PAINT COLOR CHANGES TO OCCUR AT INSIDE CORNERS UNLESS NOTED OTHERWISE.
2. ALL PAINT FINISHES ARE TO BE FLAT UNLESS NOTED OTHERWISE.
3. T.O.P. CL. = TOP OF PARAMET ELEVATION.
4. F.F. = FINISH FLOOR ELEVATION.
5. STOREFRONT CONSTRUCTION: GLASS, METAL ATTACHMENTS AND LINTELS SHALL BE DESIGNED TO RESIST 15 MPH EXPOSURE "C" WINDS. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS PRIOR TO INSTALLATION.
6. CONTRACTOR SHALL FULLY PAINT ONE CONCRETE PANEL W/ SELECTED COLORS. ARCHITECT AND OWNER SHALL APPROVE PRIOR TO PAINTING REMAINDER OF BUILDING.
7. BACK SIDE OF PARAMETS TO HAVE SMOOTH FINISH AND BE PAINTED WITH ELASTOMERIC PAINT.
8. FOR SPANDREL GLAZING, ALLOW SPACE BEHIND SPANDREL TO BREATHE.
9. USE ADHESIVE BACK WOOD STRIPS FOR ALL REVEAL FORMS.
10. THE FIRST COAT OF PAINT TO BE ROLLED-ON AND THE SECOND COAT TO BE SPRAYED-ON.

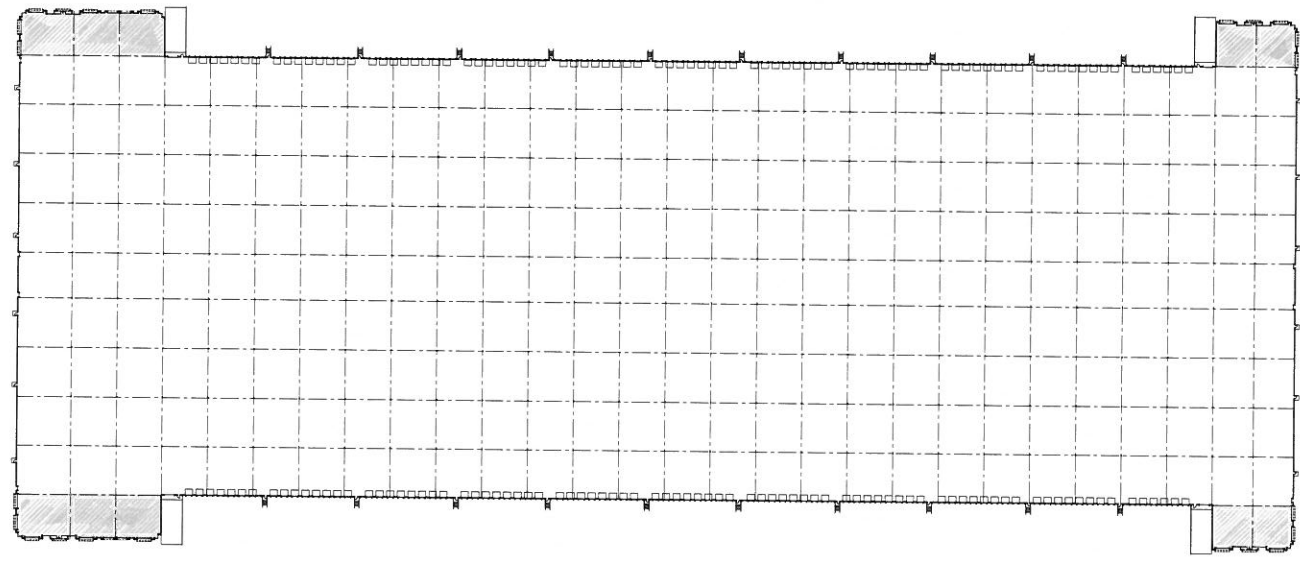
**ELEVATION COLOR LEGEND/SCHED.**

- |    |  |
|----|--|
| 1  | CONCRETE TILT-UP PANEL COLOR: SHERWIN WILLIAMS SW 7005 PURE WHITE  |
| 2  | CONCRETE TILT-UP PANEL COLOR: SHERWIN WILLIAMS SW 7031 GRAY SCREEN   |
| 3  | CONCRETE TILT-UP PANEL COLOR: SHERWIN WILLIAMS SW 7032 ONLINE  |
| 4  | CONCRETE TILT-UP PANEL COLOR: SHERWIN WILLIAMS SW 7033 NETWORK GRAY  |
| 5  | CONCRETE TILT-UP PANEL COLOR: SHERWIN WILLIAMS SW 7034 SOFTWARE  |
| 6  | CONCRETE TILT-UP PANEL COLOR: SHERWIN WILLIAMS SW 7035 WEB GRAY  |
| 7  | GLAZING COLOR: BLUE REFLECTIVE GLAZING   |
| 8  | WALLINGS COLOR: BLACK ANODIZED   |
| 9  | METAL CANOPY: SHERWIN WILLIAMS PRO-INDUSTRIAL WATER-BASED 853-1100 SEMI-GLOSS TN COLOR SW 7075 WEB GRAY    |
| 10 | DOOR COLORS: SHERWIN WILLIAMS SW 7005 PURE WHITE   |
| 11 | CONCRETE TILT-UP PANEL - 1/4" BE SING (BEASED-EDGE) LONGEST LENGTH IS 20" MINIMUM SPANS MONOLITHIC STAGING |

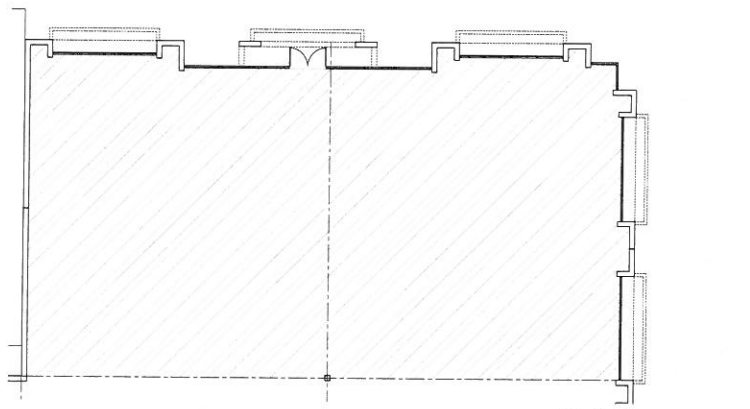
**GLAZING LEGEND**

- NOTE: ALL EXTERIOR AND INTERIOR GLAZING SHALL BE TEMPERED.
- |   |                |   |              |
|---|----------------|---|--------------|
|  | SPANDREL GLASS |  | VISION GLASS |
|---|----------------|---|--------------|
- ALL GLAZING TO BE TEMPERED
- GLAZING: PPG SOLARCOOL (2) CHARLITE II + SOLARBAN 60 (3) CLEAR  
U: 0.59 SHGC: 0.71 VLT: 85%  
T: INSULATED GLASS UNIT WITH 1/2" AIRSPACE AND (2) 1/4" LITES
- SPANDREL: 1/4" SOLARCOOL CHARLITE SPANDREL W/ HANNOVAY GRAY SPANDREL PAINTED ON REFLECTIVE.
- WALLINGS ANODIZED CLEAR





**OVERALL FLOOR PLAN**  
SCALE: 1/4" = 60'-0"  
NORTH



**OVERALL FLOOR PLAN**  
SCALE: 1/8" = 1'-0"  
NORTH

**FLOOR PLAN KEYNOTES**

1. CONCRETE TILT-UP PANEL.
2. STRUCTURAL STEEL COLUMN.
3. TYPICAL STOREFRONT SYSTEM WITH GLAZING. SEE OFFICE BLOW-UP AND DIMENSIONS FOR SIZE, COLOR AND LOCATIONS.
4. CONCRETE RAMP W/ 4" HIGH CONC TILT-UP GUARD WALL OR BUILDING WALL ON BOTH SIDES OF RAMP.
5. 3'-0" x 8'-0" x 14" THICK CONCRETE EXTERIOR LANDING PAD TYP. AT ALL EXTERIOR MAIN DOORS TO LANDSCAPED AREAS. FINISH TO BE MEDIUM BROWN FINISH. PROVIDE WALK TO PUBLIC WAY OR DRIVE WAY AS REC. BY CITY SPECIFICATION.
6. EXTERIOR CONCRETE STAIR W/ 4" HIGH CONC TILT-UP GUARD WALL OR BUILDING WALL ON BOTH SIDES OF STAIR.
7. 9' x 10' TRUCK DOOR, SECTIONAL O.H., STANDARD GRADE.
8. 6' x 8' LOUVERED OPENING FOR VENTILATION.
9. DOOR DOOR BUMPER TYPICAL.
10. 12' x 14' STEEL TRAIL SECTIONAL O.H., STANDARD GRADE.
11. 3' x 7' HOLLOW METAL EXTERIOR MAIN DOOR.
12. SLOTTED LINE ABOVE.
13. CONC. FILLED GUARD POST 5" DIA. U.N.O. 42"x4".
14. EMPLOYEE BREAK/SMOKING AREA.
15. PRE-CAST CONCRETE WHEEL STOP.
16. Z GUARD.
17. APPROXIMATE LOCATION OF ELECTRICAL ROOM.
18. METAL CANNOPY ABOVE.
19. INTERIOR BIKE RACK TYPICAL.
20. INTERIOR ROOF DRAIN W/ 2 OVERFLOW SCOURERS.

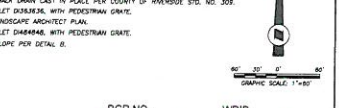
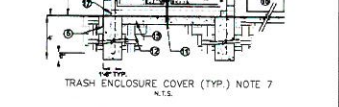
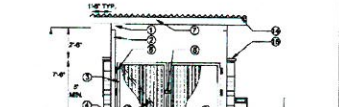
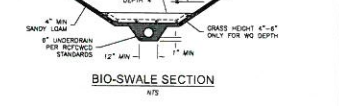
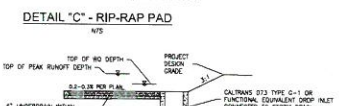
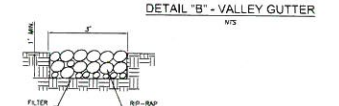
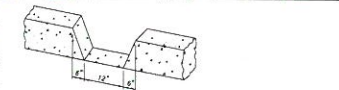
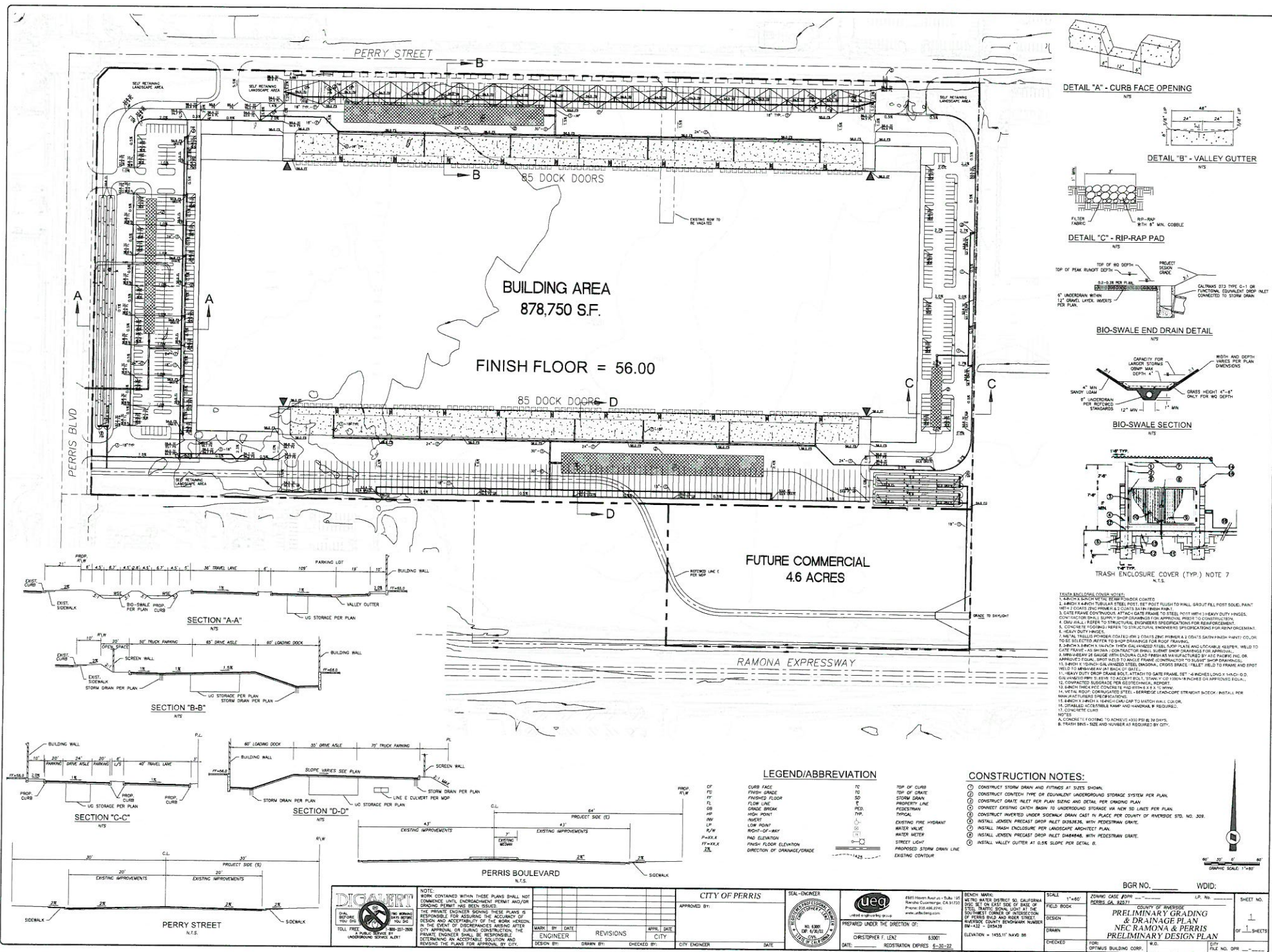
**FLOOR PLAN GENERAL NOTES**

1. THIS BUILDING IS DESIGNED FOR HIGH PILE STORAGE WITH FIRE ACCESS MAIN DOORS AT 10' MINIMUM O.C. A SEPARATE PERMIT WILL BE REQUIRED FOR ANY RACKING/CONVEYER SYSTEMS.
2. FIRE HOSE LOCATIONS SHALL BE APPROVED PER FIRE DEPARTMENT.
3. SEE '10' DRAWINGS FOR FINISH SURFACE ELEVATIONS.
4. WAREHOUSE INTERIOR CONCRETE WALLS ARE PAINTED WHITE. COLUMNS ARE TO RECEIVE TRUCKS ONLY. ALL EXP. BR. WALLS IN WAREHOUSE TO RECEIVE 1 COAT OF WHITE TO COVER.
5. SLOPE POOR STRIP 1/2" TO EXTERIOR AT ALL WAREHOUSE EXITS. SEE '5' DRAWINGS FOR POUR STRIP LOCATION.
6. ALL DIMENSIONS ARE TO THE FACE OF CONCRETE PANEL, WALL, CURBLINE, OR FACE OF STUD U.N.O.
7. SEE CIVIL DRAWINGS FOR POINT OF CONNECTIONS TO OFF-SITE UTILITIES. CONTRACTOR TO VERIFY ACTUAL UTILITY LOCATIONS. PLUMBING/ELECTRICAL COORDINATION.
8. EXH. DOOR TYPES AND SIZES. SEE DETAIL SHEET -- NOTE: ALL DOORS PER DOOR SCHEDULE ARE FINISH OPENINGS.
9. CONTRACTOR TO PROTECT AND KEEP THE FLOOR SLAB CLEAN. ALL EQUIPMENT TO BE DAMPED INCLUDING DAPS AND TRUCKS.
10. ALL EXH. MAIN DOORS IN WAREHOUSE TO HAVE ILLUMINATED EXIT SIGN HORIZONTAL.
11. HIGHLY FLAMMABLE AND COMBUSTIBLE MATERIAL SHALL NOT BE USED OR STORED IN THIS BUILDING.
12. EACH EXTERIOR EXIT DOOR SHALL BE IDENTIFIED BY A FACILE EXIT SIGN WITH THE WORDS 'EXIT' THE MOUNTING HEIGHT FOR SIGN SCHEDULE SHALL BE 60" FROM FINISH FLOOR LEVEL TO THE CENTER OF THE SIGN.
13. NON-ACCESSIBLE DOOR. PROVIDE WARNING SIGN LOCATED IN THE INTERIOR SIDE PER CBC 115.08.1(1).
14. ALL ROOF MOUNTED MATERIALS SHALL BE FULLY SCREENED FROM PUBLIC VIEW. SEE A44.1 OFFICE SECTION.

**FLOOR SLAB & POUR STRIPS REQ.**

- THESE NOTES ARE VERY MIN. REQUIREMENT. SEE '5' DWGS FOR ADDITIONAL REQUIREMENTS.
1. FLOOR CONSTRUCTION - SEE.
  2. TRENCH CONSTRUCTION - SEE.
  3. BUILDING FLOOR SLAB.
    - A. 4" THICK MIN. UNREINFORCED CONCRETE OVER COMPACTED SOILS.
    - B. 3/4" - 1" LONG @ 12" O.C. BOWLS AT ALL CONSTRUCTION JOINTS.
    - C. 1/4" - 1/2" LONG @ 24" O.C. BOWLS IN DOWEL BARREL AT ALL CONTROL JOINTS.
    - D. 4.000 P.S.I. REQUIREMENT.
    - E. SLOPE TO BE 1/4" - 1/2" TO 302-R-96.
    - F. SAW-CUT DEPTH 1/4" 1: SOT; SAW-CUTTING WITHIN 2 HRS OF FINISHING.
  4. CONTRACTOR TO BUILD FOR CLASS V FLOOR PER A.C.I. 302-R-96.
  5. CONCRETE SLAB TO HAVE STEEL FIBER HOOK BOWLS BURNISHED FINISH.
  6. CONTRACTOR TO CURE SLAB TO BE WET CURING USING BURLINE FOR 7 DAYS MIN.
  7. ALL EQUIPMENT & WORKING VEHICLES SHALL BE DAMPED.
  8. NO CRACKS. CONCRETE TRUCKS OR ANYTHING HEAVY WILL BE PLACED ON THE SLAB.
  9. SLAB TO BE FFSO FLTS MEASURED WITHIN 24 HOURS.
  10. NO RE-USE IN THE CONCRETE.
  11. WHERE [ ] INDICATED, PROVIDE VAPOR BARRIER (15#AL STEEL OR EQUAL) UNDER THE CONCRETE SLAB. PROVIDE SAND FOR SOLES ENGINEER OR MANUFACTURERS RECOMMENDATION.
  12. CONCRETE SLAB IN FUTURE OFFICE AREAS, WHERE SAND OVER VOIDEN ARE REQUIRED, SHALL BE NATURALLY HYDRATED WITHOUT USE OF BURLINE, CURING COMPOUND, OR RELEASE AGENTS. CONTROL/CONSTRUCTION JOINTS SHALL NOT BE FILLED WITH M-M-80 JOINT FILLER IN FUTURE OFFICE AREAS.
  13. SEAL CONCRETE SLAB W/ 'LIPODOLITY' SEALER.





SECTION "A-A"

SECTION "B-B"

SECTION "C-C"

SECTION "D-D"

LEGEND/ABBREVIATION

PROP. R/W	OF	CURB FACE	TC	TOP OF CURB
FF	FF	FINISH GRADE	TO	TOP OF GRADE
FL	FL	FINISHED FLOOR	SD	STORM DRAIN
FL	FL	FLOOR LINE	PR	PRIORITY LINE
GR	GR	GRADE BREAK	PD	PROPOSED
HP	HP	HIGH POINT	TP	TYPICAL
HW	HW	HEAD	EX	EXISTING
LP	LP	LOW POINT	HY	HYDRANT
MP	MP	POST-OF-WAY	WM	WATER METER
PA	PA	PAD ELEVATION	SL	STREET LIGHT
FF=XXX	FF=XXX	FINISH FLOOR ELEVATION	SD	PROPOSED STORM DRAIN LINE
SS	SS	DIRECTION OF DRAINAGE/GRADE	EX	EXISTING CONTOUR

CONSTRUCTION NOTES:

- CONSTRUCT STORM DRAIN AND FITTINGS AT SIZES SHOWN.
- CONSTRUCT CONCRETE TYPE II OR EQUIVALENT UNDERGROUND STORAGE SYSTEM PER PLAN.
- CONSTRUCT GATE INLET PER PLAN SIZE AND DETAIL PER DRAINAGE PLAN.
- CONSTRUCT UNDERGROUND STORAGE PER PLAN SIZE AND DETAIL PER PLAN.
- CONSTRUCT INVERTED UNDERSTORM DRAIN CAST IN PLACE PER COUNTY OF RIVERSIDE STD. NO. 309.
- CONNECT EXISTING CATCH BASIN TO UNDERGROUND STORAGE PER PLAN.
- INSTALL JENSEN PRECAST STORM INLET DRAINAGE WITH WEEDSTRAM GATE.
- INSTALL TRASH ENCLOSURE PER LANDSCAPE ARCHITECT PLAN.
- INSTALL JENSEN PRECAST STORM INLET DRAINAGE WITH WEEDSTRAM GATE.
- INSTALL VALLEY GUTTER AT 0.5% SLOPE PER DETAIL B.

**DIGALINI**

NOTE: WORK CONTAINED WITHIN THESE PLANS SHALL NOT COMMENCE UNLESS UNDERWRITING PERMIT AND/OR GRADING PERMIT HAS BEEN ISSUED.

THE PRIME ENGINEER SIGNING THESE PLANS IS RESPONSIBLE FOR ASSURING THE ACCURACY OF DESIGN AND ACCEPTABILITY OF THE WORK HEREIN IN THE EVENT OF DISCREPANCIES ARISING AFTER CITY APPROVAL OR DURING CONSTRUCTION. THE PRIME ENGINEER SHALL BE RESPONSIBLE FOR DETERMINING AN ACCEPTABLE SOLUTION AND REVISIONS TO THE PLANS FOR APPROVAL BY CITY.

TOLL FREE: 800-291-2888  
PHONE: 951-799-7000  
ADDRESS: 1000 S. MAIN ST., PERRIS, CA 92570

**CITY OF PERRIS**

APPROVED BY: \_\_\_\_\_

DATE: \_\_\_\_\_

DESIGN BY: \_\_\_\_\_

DRAWN BY: \_\_\_\_\_

CHECKED BY: \_\_\_\_\_

CITY ENGINEER: \_\_\_\_\_

**uea**

4665 Haven Ave., Suite 105  
Beverly Hills, CA 91605  
Phone: 310-488-0900  
www.uea.com

PREPARED UNDER THE DIRECTION OF:  
CHRISTOPHER F. LENZ  
DATE: \_\_\_\_\_

BENCH MARK: METRIC WATER DISTRICT 50, CALIFORNIA HIGHWAY 90, 1.1460'

SCALE: 1"=40'

FIELD BOOK: \_\_\_\_\_

DESIGN: \_\_\_\_\_

DRAWN: \_\_\_\_\_

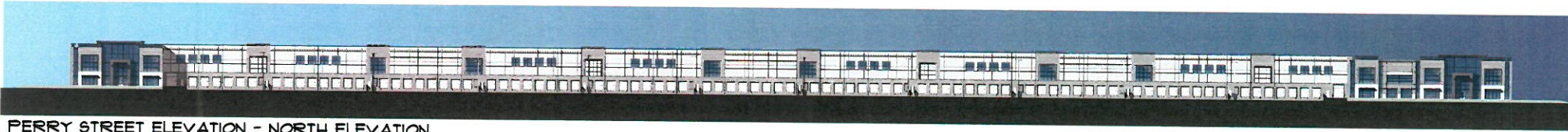
CHECKED: \_\_\_\_\_

BGR NO.: \_\_\_\_\_

WIDD: \_\_\_\_\_

SHEET NO.: \_\_\_\_\_

PROJECT: PRELIMINARY GRADING & DRAINAGE PLAN  
NEC RAMONA & PERRIS  
PRELIMINARY DESIGN PLAN



PERRY STREET ELEVATION - NORTH ELEVATION



EAST ELEVATION



RAMONA EXPRESSWAY ELEVATION - SOUTH ELEVATION



PERRY BLVD. ELEVATION - WEST ELEVATION



CONCEPTUAL ELEVATIONS - 40' CLEAR  
RAMONA EXPY. & PERRIS BLVD.  
PERRIS, CA

OPTIMUS GROUP

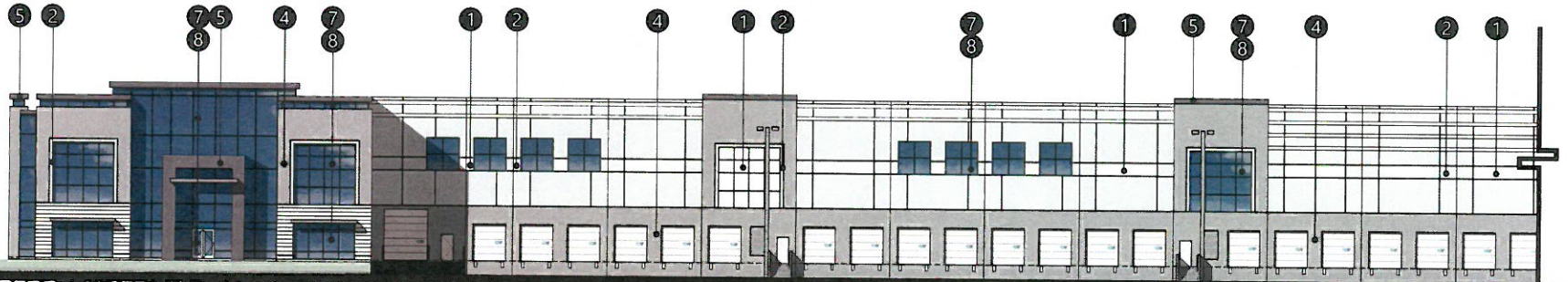


PERRY STREET ELEVATION - NORTH ELEVATION



EAST ELEVATION

- |   |  |                                       |   |   |   |                            |                            |   |    |    |
|---|--|---------------------------------------|---|---|---|----------------------------|----------------------------|---|----|----|
| 1   | 2  | 3                                     | 4   | 5                                       | 6                                       | 7                          | 8                          | 9 | 10 | 11 |
|   |  |                                       |   |   |   |                            |                            |   |    |    |
| SHERWIN WILLIAMS<br>SW 7005<br>PURE WHITE | SHERWIN WILLIAMS<br>SW 7071<br>GRAY SCREEN | SHERWIN WILLIAMS<br>SW 7072<br>ONLINE | SHERWIN WILLIAMS<br>SW 7073<br>NETWORK GRAY | SHERWIN WILLIAMS<br>SW 7074<br>SOFTWARE | SHERWIN WILLIAMS<br>SW 7075<br>WEB GRAY | BLUE REFLECTIVE<br>GLAZING | CLEAR ANODIZED<br>MULLIONS |   |    |    |



ENLARGED VIEW OF PERRY STREET ELEVATION - NORTH ELEVATION

JOB NO. 202005

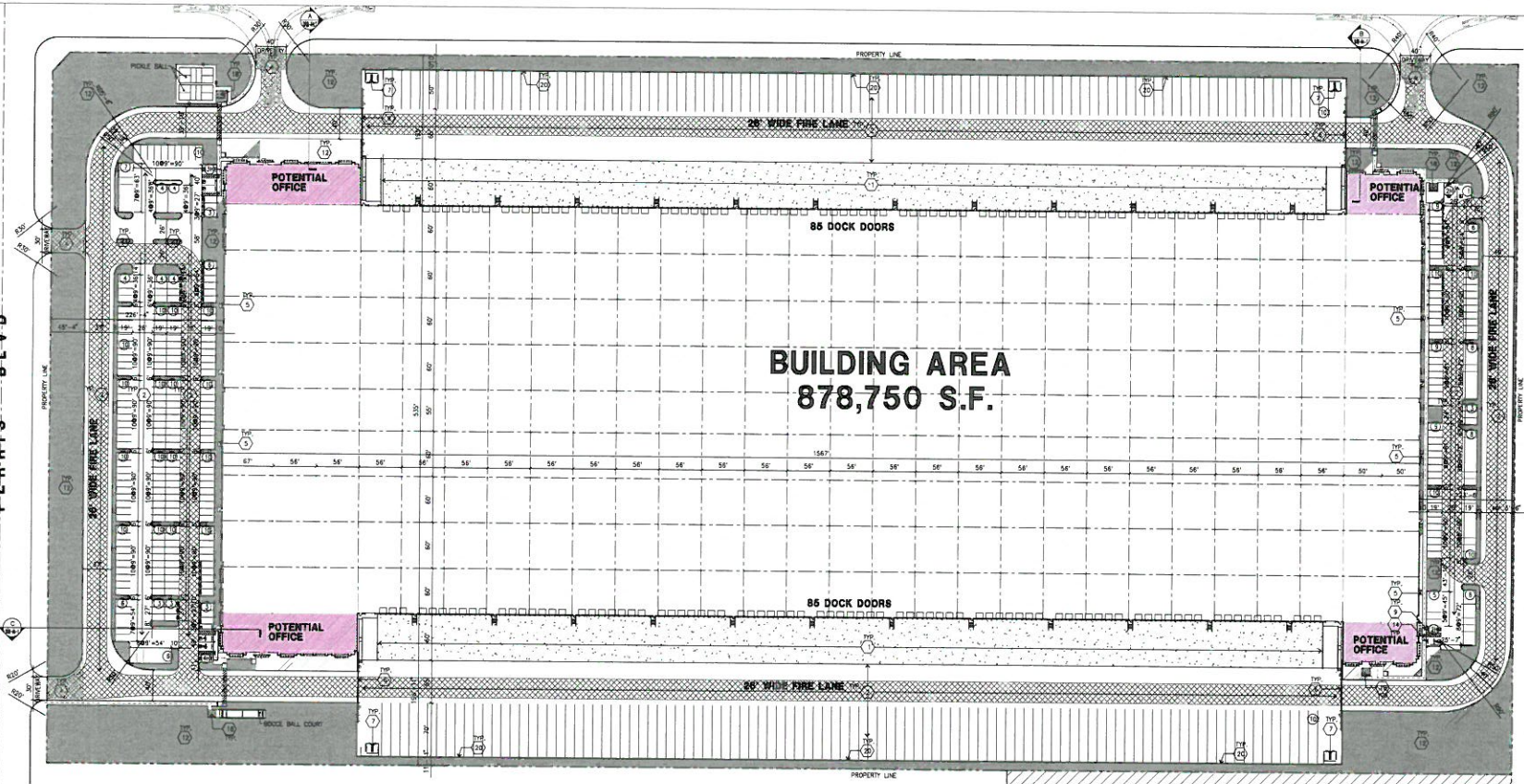


CONCEPTUAL ELEVATIONS & MATERIAL BOARD  
**RAMONA EXPY. & PERRIS BLVD.**  
 PERRIS, CA

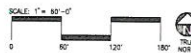
JANUARY 22, 2022

**OPTIMUS GROUP**

PERIS BLVD



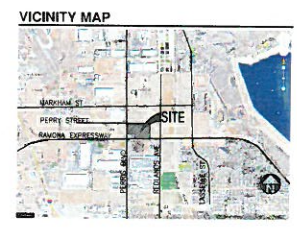
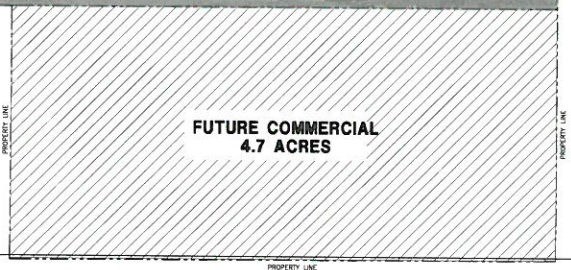
OVERALL SITE PLAN A  
SCALE: 1" = 50'-0"



PROJECT DATA

<b>NET AREA</b>	
In s.f.	1,775,193 s.f.
In acres	40.75 ac
<b>REAL ESTATE</b>	
Office - 1st floor	10,000 s.f.
Office - 2nd floor	10,000 s.f.
Warehouse	858,750 s.f.
TOTAL	878,750 s.f.
<b>COVERAGE</b>	
	48.5%
<b>AUTO PARKING REQUIRED</b>	
High Code	
Office 1,000 s.f. (if exceeds 10% of GFA)	n/a
Warehouse 1st 20k @ 17,000 s.f.	20 stalls
2nd 20k @ 12,000 s.f.	10 stalls
above 40k @ 1,500 s.f.	163 stalls
Total parking requires	193 stalls
<b>AUTO PARKING PROVIDED</b>	
High Code	
Standard ADA	302 stalls
ADA VAN	6 stalls
Standard BV	2 stalls
ADA BV	33 stalls
ADA BV	1 stall
ADA VAN	1 stall
ADA Ambulatory	1 stall
Clean Air / VAN pool	7 stalls
Total	353 stalls
<b>TRAILER PARKING PROVIDED</b>	
Trailer (12 x 43)	213 stalls
<b>ZONING ORDINANCE FOR CITY</b>	
Zoning Designation - Light Industrial (LI)	
MAXIMUM BUILDING HEIGHT ALLOWED	Height - 50'
<b>MAXIMUM BUILDING COVERAGE</b>	
Coverage - 15%	
<b>LANDSCAPE REQUIREMENT</b>	
Percentage - 12%	
<b>LANDSCAPE PROVIDED</b>	
Percentage	12.1%
In s.f.	215,000 s.f.
<b>SETBACKS</b>	
Perry Street - 10'	
Ramona Expy - 20'	
Perris Blvd - 25'	
Side/Back - None	

POTENTIAL OFFICE LOCATIONS WITH POTENTIAL MEZZANINE OFFICES. NOT TO EXCEED 10,000 SF ON GROUND LEVEL AND 10,000 SF ON MEZZANINE LEVEL. THIS IS A PROPOSAL FOR A SPEC BUILDING WITH SHELL AND CORE ONLY.



OFFICIAL USE ONLY

CAUTION : IF THIS SHEET IS NOT A 30" X 42" IT IS A REDUCED PRINT

Title: OVERALL SITE PLAN

Project Number: 20505

Drawn by: RC

Date:

Revisions:

Sheet: DAB-A1.1



hpa, inc.  
18831 barden avenue - ste.  
#100 Irvine, CA  
92612  
tel: 949-863-1770  
fax: 949-863-0851  
email: hpa@hpaarch.com

Owner:  
OPTIMUS GROUP

Project:  
RAMONA EXPY. & PERRIS BLVD.

Consultants:  
Civil:  
Structural:  
Mechanical:  
Plumbing:  
Electrical:  
Landscape:  
Fire Protection:  
Traffic Engineer:

Title: OVERALL SITE PLAN

Project Number: 20505

Drawn by: RC




Date:

Revisions:

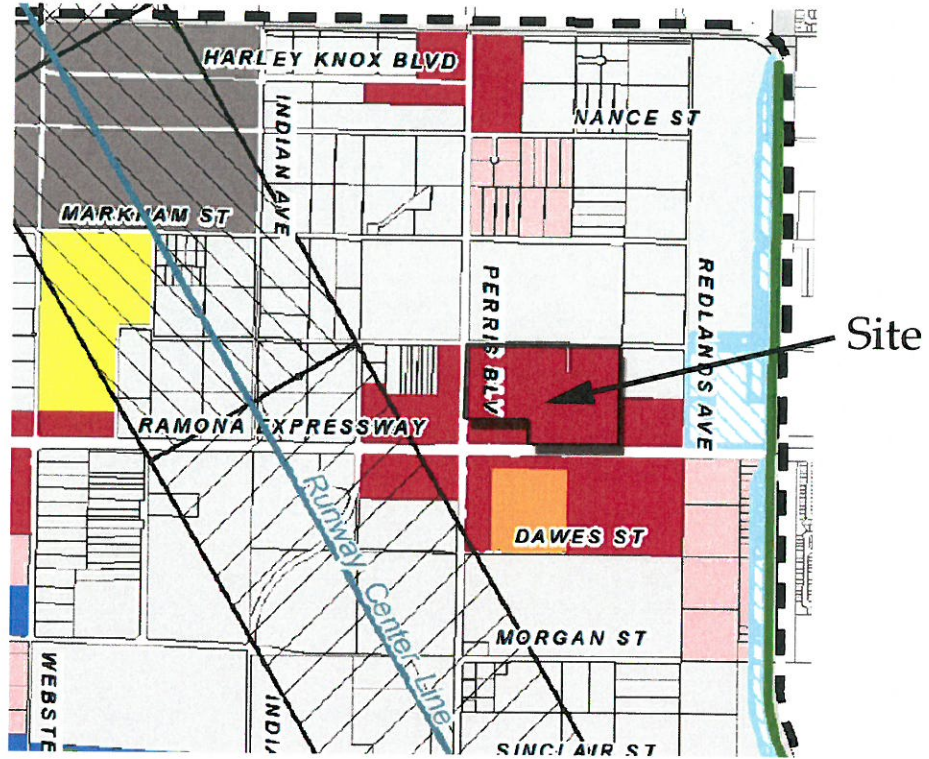
Sheet: DAB-A1.1



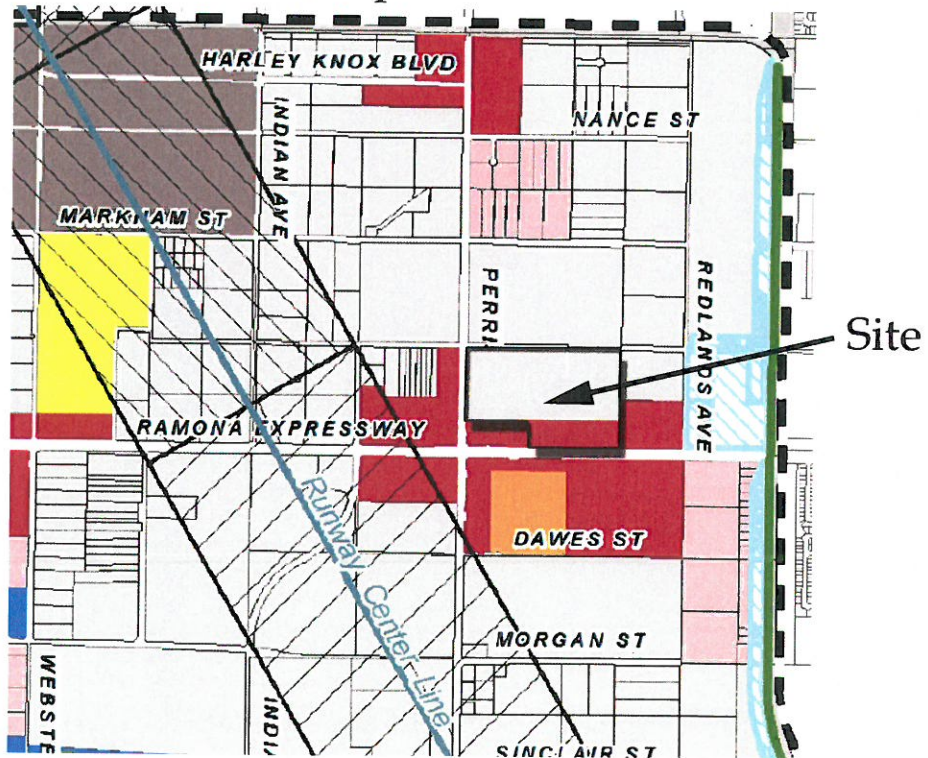
**LEGEND**

-  SPECIFIC PLAN BOUNDARY
-  POTENTIAL BASIN AREAS
-  COMMERCIAL
-  FUTURE PERRIS VALLEY STORM DRAIN
-  BUSINESS PROFESSIONAL OFFICE
-  CLEAR ZONE
-  LIGHT INDUSTRIAL
-  ACCIDENT POTENTIAL ZONE I
-  PROPOSED LAND USE RESIDENTIAL
-  GENERAL INDUSTRIAL
-  ACCIDENT POTENTIAL ZONE II
-  PROPOSED LAND USE MULTI-FAMILY RESIDENTIAL
-  PUBLIC/SEMI-PUBLIC FACILITY
-  TRAIL

Existing



Proposed



**NOTICE OF PUBLIC HEARING**  
**RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**  
[www.rcaluc.org](http://www.rcaluc.org)

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. **Information on how to participate in the hearing will be available on the ALUC website at [www.rcaluc.org](http://www.rcaluc.org).** The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893.**

The City of Perris Planning Department should be contacted on non-ALUC issues. For more information, please contact City of Perris Planner Mr. Mathew Evans at 951-943-6100.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website [www.rcaluc.org](http://www.rcaluc.org). Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to [prull@rivco.org](mailto:prull@rivco.org). Individuals with disabilities requiring reasonable modifications or accommodations, please contact Barbara Santos at (951) 955-5132.

**PLACE OF HEARING:** Riverside County Administration Center  
4080 Lemon Street, 1<sup>st</sup> Floor Board Chambers  
Riverside California

**DATE OF HEARING:** May 12, 2022

**TIME OF HEARING:** 9:30 A.M.

**CASE DESCRIPTION:**

ZAP1516MA22 – Optimus Building Corporation (Representative: Mike Naggar & Associates) – City of Perris Case Nos. SPA22-05047 (Specific Plan Amendment), DPR22-00006 (Development Plan Review). A proposal to construct an 878,750 square foot industrial manufacturing building with mezzanines on a 40.75 acres located northerly of Ramona Expressway, westerly of Redlands Avenue, easterly of Perris Boulevard, and southerly of Perry Street (approximately 4.5 acres of the site is planned for future commercial development – there are no entitlements proposed for this area at this time). The applicant also proposes amending the Perris Valley Commerce Center Specific Plan rezoning the site from Commercial to Light Industrial (Airport Compatibility Zones C1 and D of the March Air Reserve Base/Inland Port Airport Influence Area).



# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

Zone D+CI March

## APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP1516MA2 DATE SUBMITTED: 3.29.22

### APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

Applicant	<u>Optimus Building Corporation c/o Mike Naggar &amp; Associates</u>	Phone Number	<u>9515517730</u>
Mailing Address	<u>445 S. D St. Perris, CA 92570</u>	Email	<u>mike@mikenaggar.com</u> ✓

Representative	<u>Mike Naggar</u>	Phone Number	<u>Same as above</u> ✓
Mailing Address	<u>same</u>	Email	

Property Owner	<u>Optimus Building Corporation</u>	Phone Number	<u>9515517730</u>
Mailing Address	<u>445 S. D St. Perris, CA 92570</u>	Email	<u>SAME</u> ✓

### LOCAL JURISDICTION AGENCY

Local Agency Name	<u>City of Perris</u>	Phone Number	<u>9519436100</u>
Staff Contact	<u>Mathew Evens</u>	Email	<u>mevans@cityofperris.org</u> ✓
Mailing Address	<u>101 N D St. Perris, CA 92570</u>	Case Type	

Local Agency Project No	<u>DPR 22-00006, <del>TPM 22-05048</del>, SPA 22-05047</u>	<input checked="" type="checkbox"/> General Plan / Specific Plan Amendment <input type="checkbox"/> Zoning Ordinance Amendment <input type="checkbox"/> Subdivision Parcel Map / Tentative Tract <input checked="" type="checkbox"/> Use Permit <input checked="" type="checkbox"/> Site Plan Review/Plot Plan <input type="checkbox"/> Other
-------------------------	--	--

### PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address	<u>N/A</u>		
Assessor's Parcel No.	<u>302-130-002,008, 018, 021, 022, 023, 024, 027</u>	Gross Parcel Size	<u>40.5 AC</u>
Subdivision Name		Nearest Airport and distance from Airport	<u>MARCH AFB</u>
Lot Number			

### PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe)	<u>SPA FROM COMMERCIAL TO LIGHT INDUSTRIAL WITH 4.7 ACRES OF REMNENT COMMERCIAL</u>



Proposed Land Use (describe)	<b>DISTRIBUTION HIGH CUBE</b>	
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)	_____
For Other Land Uses (See Appendix C)	Hours of Operation	_____
	Number of People on Site	Maximum Number _____
	Method of Calculation	_____
Height Data	Site Elevation (above mean sea level)	1459 ft.
	Height of buildings or structures (from the ground)	50 ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
	If yes, describe	_____ _____ _____

- A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:**
1. . . . . Completed ALUC Application Form
  1. . . . . ALUC fee payment
  1. . . . . Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
  1. . . . . Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
  1. . . . . CD with digital files of the plans (pdf)
  1. . . . . Vicinity Map (8.5x11)
  1. . . . . Detailed project description
  1. . . . . Local jurisdiction project transmittal
  3. . . . . Gummed address labels for applicant/representative/property owner/local jurisdiction planner
  3. . . . . Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. (Only required if the project is scheduled for a public hearing Commission meeting)

# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

## STAFF REPORT

**AGENDA ITEM:** 3.5

**HEARING DATE:** May 12, 2022

**CASE NUMBER:** ZAP1515MA22 - Meridian Park West, LLC (Representative: Waypoint Property Group)

**APPROVING JURISDICTION:** March Joint Powers Authority

**JURISDICTION CASE NO:** GP 21-01 (General Plan Amendment), SP21-01 (Specific Plan), PP21-03 and PP21-04 (Plot Plans), TTM38063 (Tentative Tract Map)

**LAND USE PLAN:** 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

**Airport Influence Area:** March Air Reserve Base

**Land Use Policy:** Zones B1, B2, C1, C2 (High Terrain Zone)

**Noise Levels:** Between 60 - 70 CNEL contour (with a portion below 60 CNEL contour)

**MAJOR ISSUES:** None

**RECOMMENDATION:** Staff recommends that the Commission find the proposed General Plan Amendment and Specific Plan CONSISTENT with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, and also find the proposed Plot Plans and Tentative Tract Map CONDITIONALLY CONSISTENT, subject to the conditions included herein, and such additional conditions as may be required by the Federal Aviation Administration Obstruction Evaluation Service.

**PROJECT DESCRIPTION:** The applicant proposes the West Campus Upper Plateau Specific Plan, encompassing 817.9 acres within multiple Airport Compatibility Zones located southerly of Alessandro Boulevard, westerly of Meridian Parkway, northerly of Grover Community Drive, and easterly of Trautwein Road. As part of this project, the applicant proposes amending the General Plan land uses to increase Parks/Recreation and Open Space from 122 acres to 453 acres, eliminate approximately 622.5 acres of Business Park, eliminate approximately 63 acres of Industrial property, approve a 2.6 acre Public Facility area for an existing water tank, and adopt the West Campus Upper Plateau Specific Plan (SP-) on approximately 351 acres and creating policies for the future recordation of a 445 acre Conservation Area. The applicant also proposes to adopt Specific Plan SP-9 containing development standards, design guidelines, infrastructure master plans, maintenance responsibilities, phasing schedule, and implementation procedures necessary to develop a 359 acre business park and adjacent park space. The Specific Plan proposes 43.1 acres of Mixed Use, 66.4 acres of Business Park, 143.3 acres of Industrial, 28.9 acres of streets

and roadways, 10 acres of developed Parks/Recreation/Open Space, 64.5 acres of undeveloped Parks/Recreation/Open Space, and 3.5 acres of Public Facilities. The Specific Plan will adopt zoning on the properties consistent with the Specific Plan land use designations. The applicant also proposes to construct 2 industrial buildings with mezzanines on separate parcels totaling 1,820,000 square feet on (combined) 115.88 acres, located northerly of (future roads) Bunker Hill Drive, easterly of Airman Drive, southerly of Arclight Drive, and westerly of Linebacker Drive. (Only development entitlements for PP21-03 and PP21-04 have been submitted with this application. No development projects for the other parcels have been proposed at this time.) The applicant also proposes a tentative tract map to divide 359.6 acres into 17 buildable lots and 7 lettered lots for streets/open space.

**PROJECT LOCATION:** The overall 817.9 acres project area is located southerly of Alessandro Boulevard, westerly of Meridian Parkway, northerly of Grover Community Drive, and easterly of Trautwein Road. The proposed 2 industrial buildings are located approximately 9,821 feet northwesterly of the northerly end of Runway 14-32 at March Air Reserve Base.

**BACKGROUND:**

Non-Residential Average Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the overall project boundary is located within Zones B1 (22.22 acres), B2 (15 acres), C1 (306 acres) and C2 (483 acres). However, the only physical development proposed at this time (2 industrial buildings) are located entirely within Compatibility Zones C1 and C2, which limits average intensity to 100 people per acre and 200 people per acre respectively.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan (ALUCP) and the Additional Compatibility Policies included in the March ALUCP, the following rates were used to calculate the occupancy for the proposed project:

- Manufacturing – 1 person per 200 square feet,
- Warehouse – 1 person per 500 square feet, and
- Office – 1 person per 200 square feet.

The project proposes to construct 2 industrial buildings with mezzanines totaling 1,820,000 square feet on separate parcels in separate compatibility zones (C1 and C2). An individual lot-by-lot analysis is included below:

- Building B, Parcel 17 (59.61 acres) in Zone C2, proposes a 1,260,000 square foot industrial manufacturing building, which includes 1,240,000 square feet of manufacturing area, 10,000 square feet of first floor office area, and 10,000 square feet of second floor office mezzanine area, accommodating an occupancy of 6,300 people, and resulting in an average intensity of 106 people per acre, which is consistent with the Compatibility Zone C2 average intensity criterion of 200 people per acre.
- Building C, Parcel 5 (27.58 acres) in Zone C1, proposes a 560,000 square foot industrial warehouse building, which includes 540,000 square feet of warehouse area, 10,000 square feet of first floor office area, and 10,000 square feet of second floor office mezzanine area, accommodating an occupancy of 1,180 people, and resulting in an average intensity of 43 people per acre, which is consistent with the Compatibility Zone C1 average intensity

criterion of 100 people per acre.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle and 1.0 persons per trailer truck space). An individual lot-by-lot analysis is included below:

- Building B. 545 standard vehicles and 471 trailer spaces, accommodating a total occupancy of 1,289 people, resulting in an average intensity of 22 people per acre, which is consistent with the Compatibility Zone C2 average intensity criterion of 200 people per acre.
- Building C. 465 standard vehicles and 113 trailer spaces, accommodating a total occupancy of 811 people, resulting in an average intensity of 29 people per acre, which is consistent with the Compatibility Zone C1 average intensity criterion of 100 people per acre.

Non-Residential Single-Acre Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the overall project boundary is located within Zones B1 (22.22 acres), B2 (15 acres), C1 (306 acres) and C2 (483 acres). However, the only physical development proposed at this time (2 industrial buildings) are located entirely within Compatibility Zones C1 and C2, which limits maximum single-acre intensity to 250 people per acre and 500 people per acre respectively. There are no risk-reduction design bonuses available, as March Air Reserve Base/Inland Port Airport is primarily utilized by large aircraft weighing more than 12,500 pounds.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre area for each of the buildings are as follows:

- Building B includes 33,560 square feet of manufacturing area, 10,000 square feet of first floor office area, 10,000 square feet of second floor office mezzanine area, resulting in a single acre intensity of 268 people, which is consistent with the Compatibility Zone C2 single acre intensity criterion maximum of 500 people.
- Building C includes 33,560 square feet of warehouse area, 10,000 square feet of first floor office area, 10,000 square feet of second floor office mezzanine area, resulting in a single acre intensity of 167 people, which is consistent with the Compatibility Zone C1 single acre intensity criterion maximum of 250 people.

March Air Reserve Base/United States Air Force Input: Given that the project's vicinity is located near the primary runway at March Air Reserve Base, the March Air Reserve Base staff was notified of the project and sent a package of plans for their review. As of the time this staff report was prepared, comments were still pending from the Air Force.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zones B1, B2, C1, and C2. The portions of the overall 817-acre site that are located within Zones B1 and B2 are designated Open Space – Conservation, which will remain passive with no development. The proposed 2 industrial buildings (and future development parcels) are located within Zones C1 and C2 where industrial development is permitted.

Noise: Although the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

depicts the entire 817-acre project boundary as being between the 60 - 70 CNEL range from aircraft noise (with a portion below the 60 CNEL contour), the location of the proposed 2 industrial buildings (and other future industrial developable parcels) are entirely located between the 60 – 65 CNEL contour range from aircraft noise (with a portion below the 60 CNEL contour). While the proposed 2 industrial buildings are not a noise-sensitive use and would not require special measures to mitigate aircraft-generated noise, such measures may be required to achieve reduced interior noise levels of 45 dBA CNEL in office areas as required pursuant to the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan.

Part 77: The elevation of Runway 14-32 at its northerly terminus is 1,535 feet above mean sea level (AMSL). At a distance of approximately 9,821 feet from the project to the nearest point on the runway, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,633 feet AMSL. The site's finished floor elevation is 1,726 feet AMSL and proposed building height is 50 feet, resulting in a top point elevation of 1,776 feet AMSL. Therefore, review of the buildings for height/elevation reasons by the FAA Obstruction Evaluation Service (FAAOES) was required (the project is located within the High Terrain Zone [HTZ], which automatically requires FAA OES review). The applicant has submitted Form 7460-1, and FAA OES has assigned Aeronautical Study Nos. 2022-AWP-2725-OE thru 2022-AWP-2730-OE, 2022-AWP-2732-OE and 2022-AWP-2733-OE to this project. Its status is currently a "work in progress".

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33C).

Although the nearest portion of the proposed project is located within 10,000 feet of the runway (approximately 9,821 feet), the project utilizes underground detention systems which will not contain surface water or attract wildlife and, therefore, would not constitute a hazard to flight.

General Plan Amendment/Specific Plan: The applicant proposes amending the General Plan land uses to increase Parks/Recreation and Open Space from 122 acres to 453 acres, eliminate approximately 622.5 acres of Business Park, eliminate approximately 63 acres of Industrial property, approve a 2.6 acre Public Facility area for an existing water tank, and adopt the West Campus Upper Plateau Specific Plan (SP-9) on approximately 351 acres and creating policies for the future recordation of a 445 acre Conservation Area. The applicant also proposes to adopt Specific Plan SP-9 containing development standards, design guidelines, infrastructure master plans, maintenance responsibilities, phasing schedule, and implementation procedures necessary to develop a 359 acre business park and adjacent park space. The Specific Plan proposes 43.1 acres of Mixed Use, 66.4 acres of Business Park, 143.3 acres of Industrial, 28.9 acres of streets and roadways, 10 acres of developed Parks/Recreation/Open Space, 64.5 acres of undeveloped Parks/Recreation/Open Space, and 3.5 acres of Public Facilities. The Specific Plan will adopt zoning on the properties consistent with the Specific Plan land use designations. The Specific Plan

also contains language requiring project's conformity to the 2014 March ALUCP.

It is important to note that although the project's boundary is located within Zones B1 (22.22 acres), B2 (15 acres), C1 (306 acres) and C2 (483 acres), only developable land use designations are exclusively located within Zones C1 and C2. The proposed Open-Space Conservation Area land use designation is the nearest portion of the project to the Base, and is located within Zones B1 and B2. The area will be used as biological and wildlife conservation, with wildlife crossings, and recreational trails.

The proposed amendments would be consistent with the Compatibility Plan as long as the underlying development's intensity is consistent with the compatibility criteria.

**CONDITIONS:**

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight or circling climb following takeoff or toward an aircraft engaged in a straight or circling final approach toward a landing at an airport, other than a DoD or FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight or circling climb following takeoff or towards an aircraft engaged in a straight or circling final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, congregate care facilities, places of assembly (including but not limited to places of worship and theaters)
  - (f) Highly noise-sensitive outdoor nonresidential uses. Examples of noise-sensitive outdoor nonresidential uses that are prohibited include, but are not limited to, major spectator-oriented sports stadiums, amphitheaters, concert halls and drive-in theaters.

(g) Other Hazards to flight.

3. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property, and be recorded as a deed notice.
4. The project has been conditioned to utilize underground detention systems, which shall not contain surface water or attract wildlife. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at [RCALUC.ORG](http://RCALUC.ORG) which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

5. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
6. The project has been evaluated to construct 2 industrial buildings with mezzanines on separate parcels totaling 1,820,000 square feet. Any increase in building area, change in use to any higher intensity use, change in building location, or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.
7. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission and March Air Reserve Base.

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



# NOTICE

**THERE IS AN AIRPORT NEARBY.  
THIS STORM WATER BASIN IS DESIGNED TO HOLD  
STORM WATER FOR ONLY 48 HOURS AND  
NOT TO ATTRACT BIRDS**

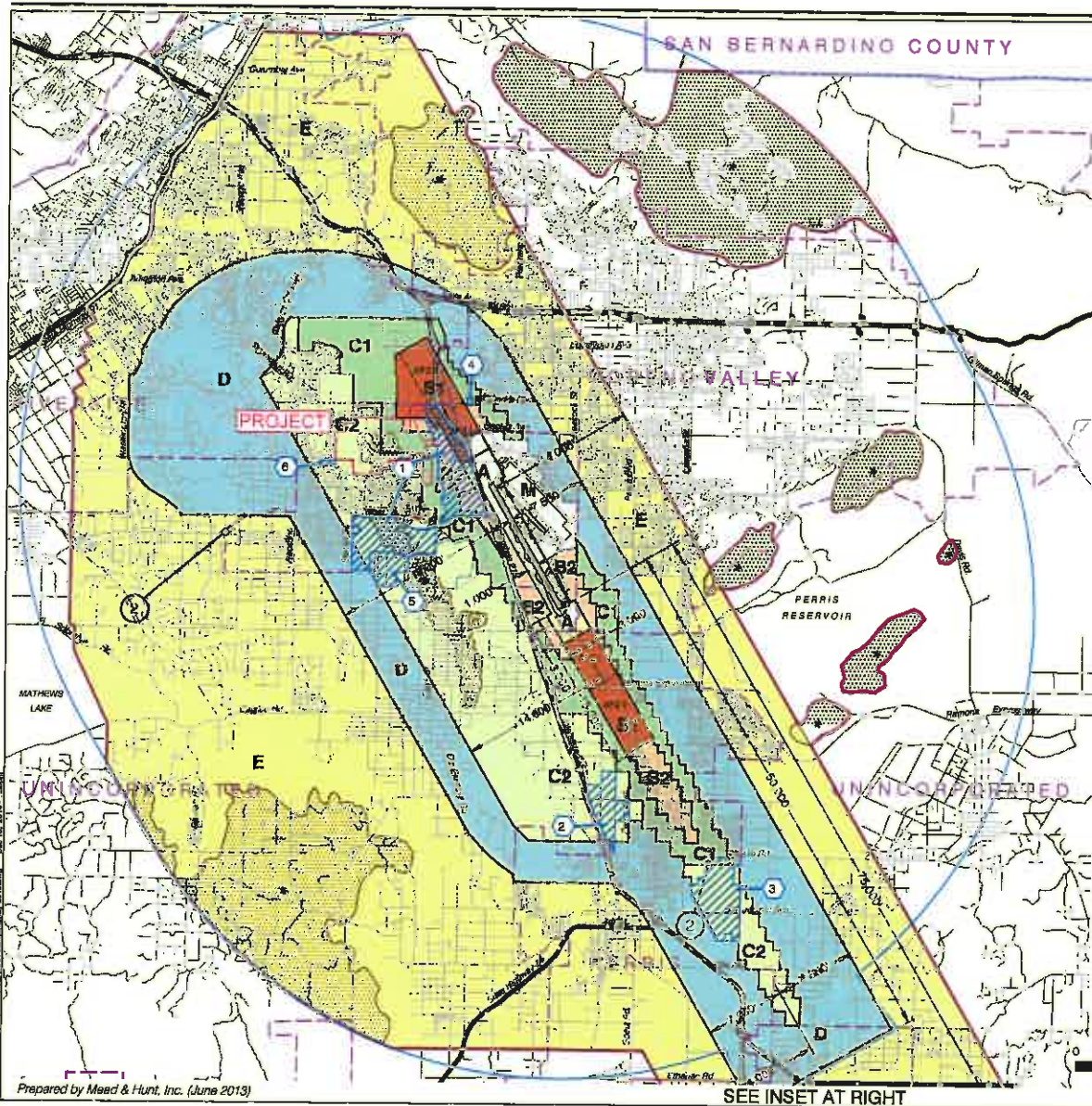
**PROPER MAINTENANCE IS NECESSARY TO AVOID  
BIRD STRIKES**



**IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:**

**Name:** \_\_\_\_\_

**Phone:** \_\_\_\_\_



**LEGEND**

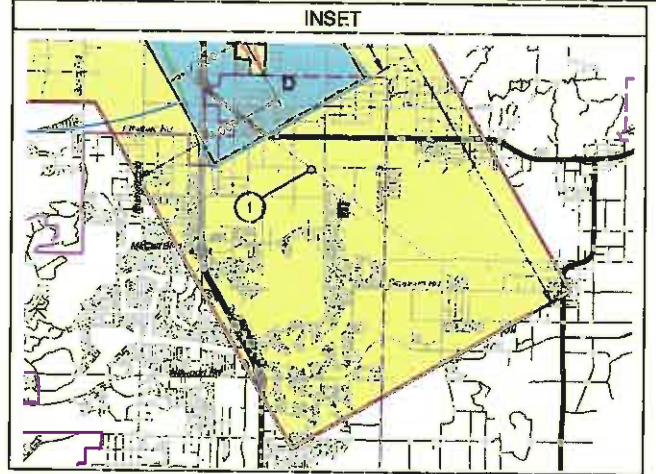
**Compatibility Zones**

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

**Boundary Lines**

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

- 1 Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- 2 Point at which departing aircraft typically reach 3,000 feet above runway end.
- 1 March JPA: March Business Center/Meridian
- 2 Perris: Harvest Landing
- 3 Perris: Park West
- 4 Moreno Valley: Affordable Housing
- 5 March JPA: Ban Clark Training Center
- 6 Riverside: Ridge Crest Subdivision



**Riverside County  
Airport Land Use Commission  
March Air Reserve Base / Inland Port Airport  
Land Use Compatibility Plan  
(Adopted November 13, 2014)**

Note:  
All dimensions are measured from  
runway ends and centerlines.



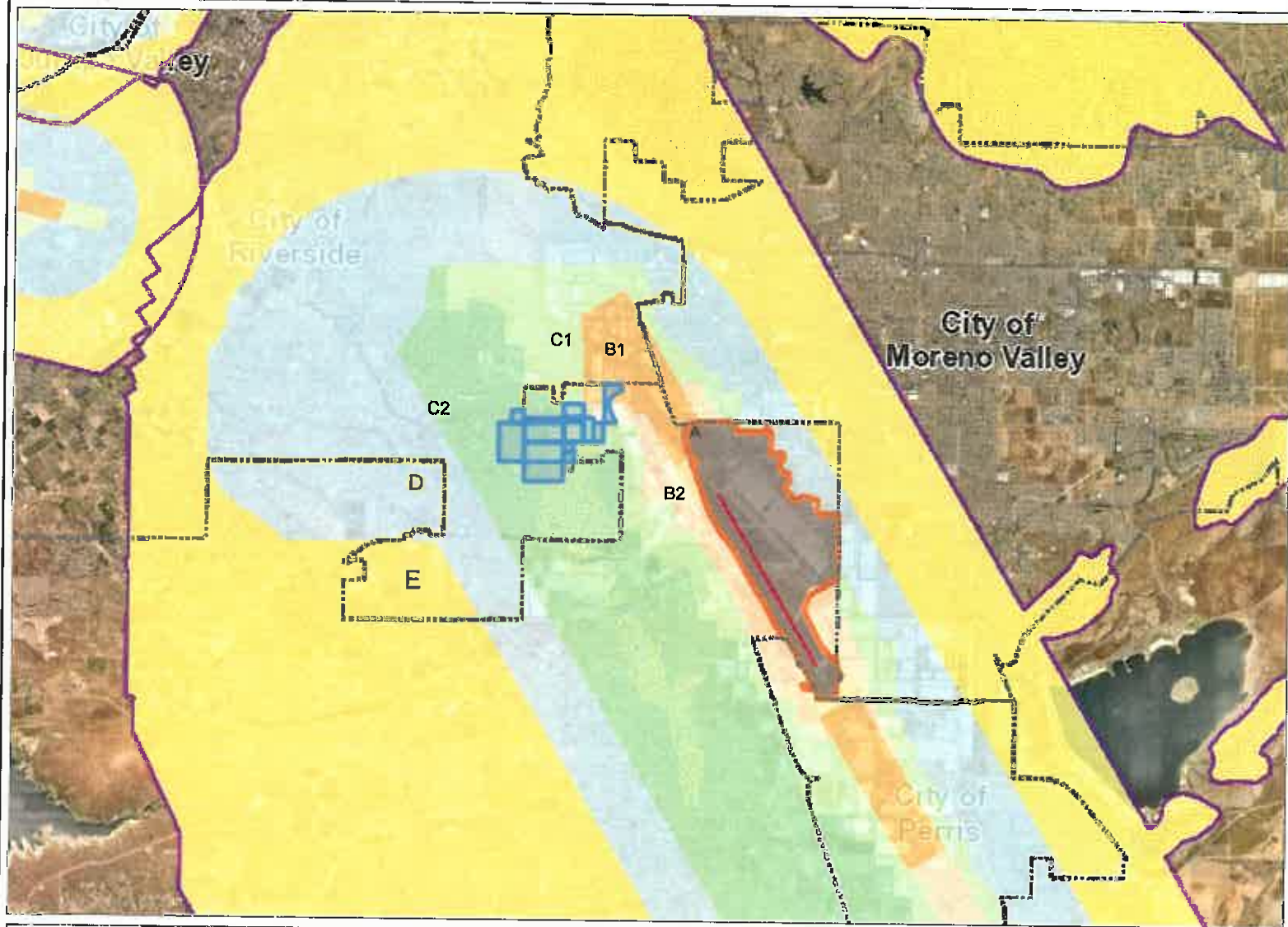
Base map source: County of Riverside 2013

Map MA-1  
**Compatibility Map**  
March Air Reserve Base / Inland Port Airport

Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

# Map My County Map



### Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

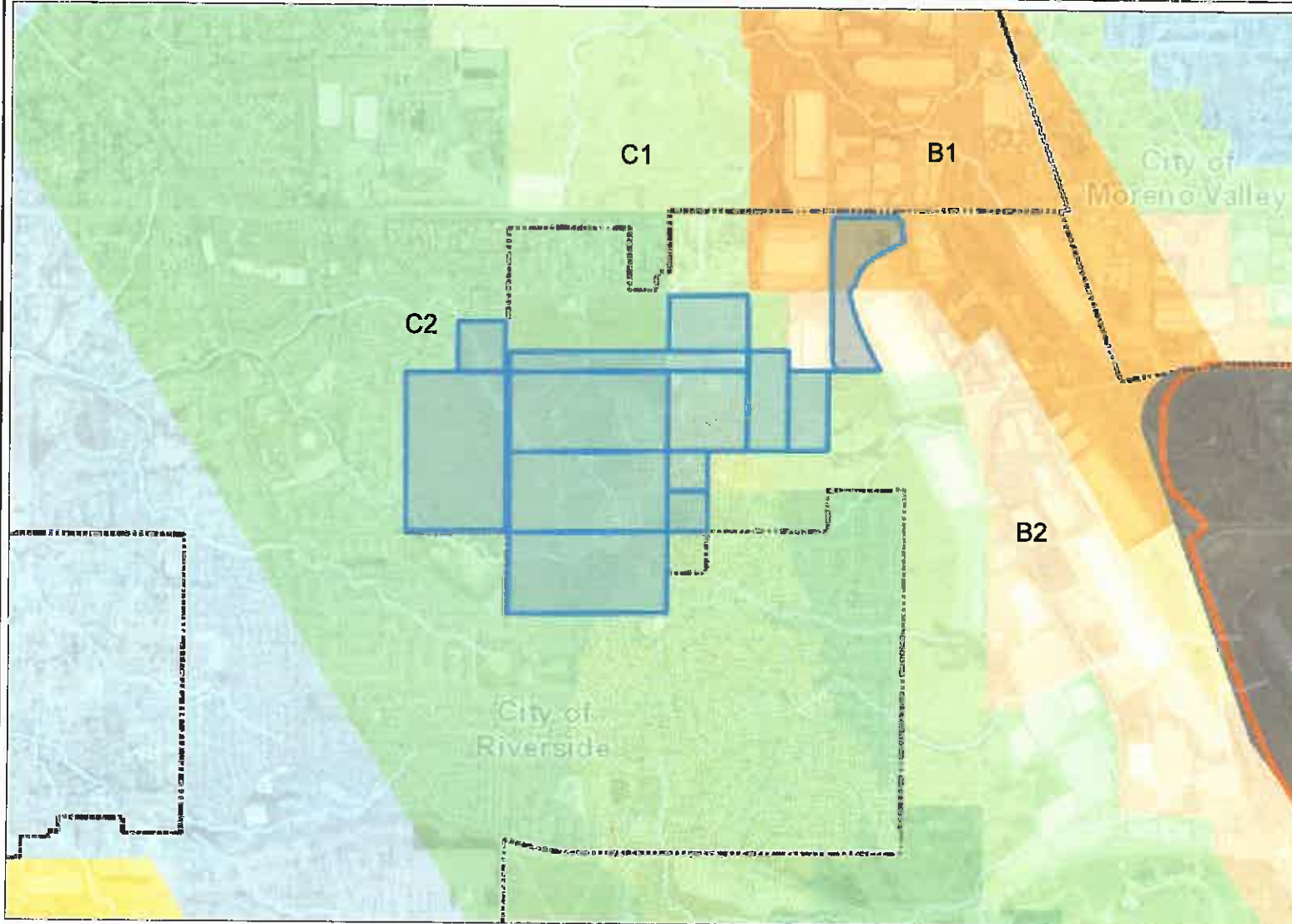
### Notes



REPORT PRINTED ON... 3/21/2022 3:36:52 PM

© Riverside County GIS

# Map My County Map



- Legend**
- Runways
  - Airports
  - Airport Influence Areas
  - Airport Compatibility Zones**
  - OTHER COMPATIBILITY ZONE
  - A
  - A-EXC1
  - B1
  - B1-APZ I
  - B1-APZ I-EXC1
  - B1-APZ II
  - B1-APZ II-EXC1
  - B1-EXC1
  - B2
  - B2-EXC1
  - C
  - C1
  - C1-EXC1
  - C1-EXC3
  - C1-EXC4
  - C1-HIGHT
  - C2
  - C2-EXC1
  - C2-EXC2
  - C2-EXC3
  - C2-EXC5
  - C2-EXC6



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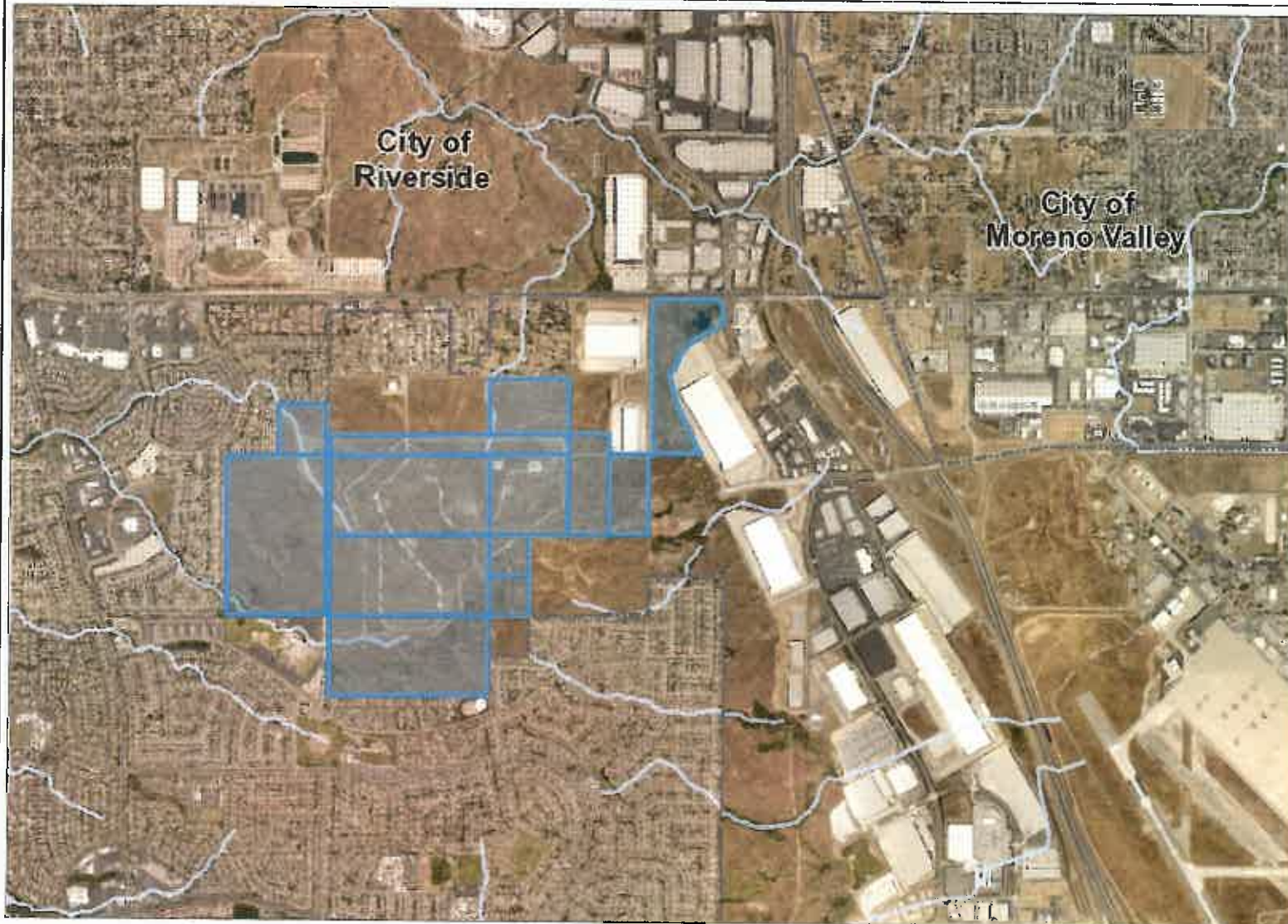


REPORT PRINTED ON... 3/21/2022 3:41:12 PM



© Riverside County GIS

## Notes

# Map My County Map



## Legend

-  Blueline Streams
-  City Areas
-  World Street Map



**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

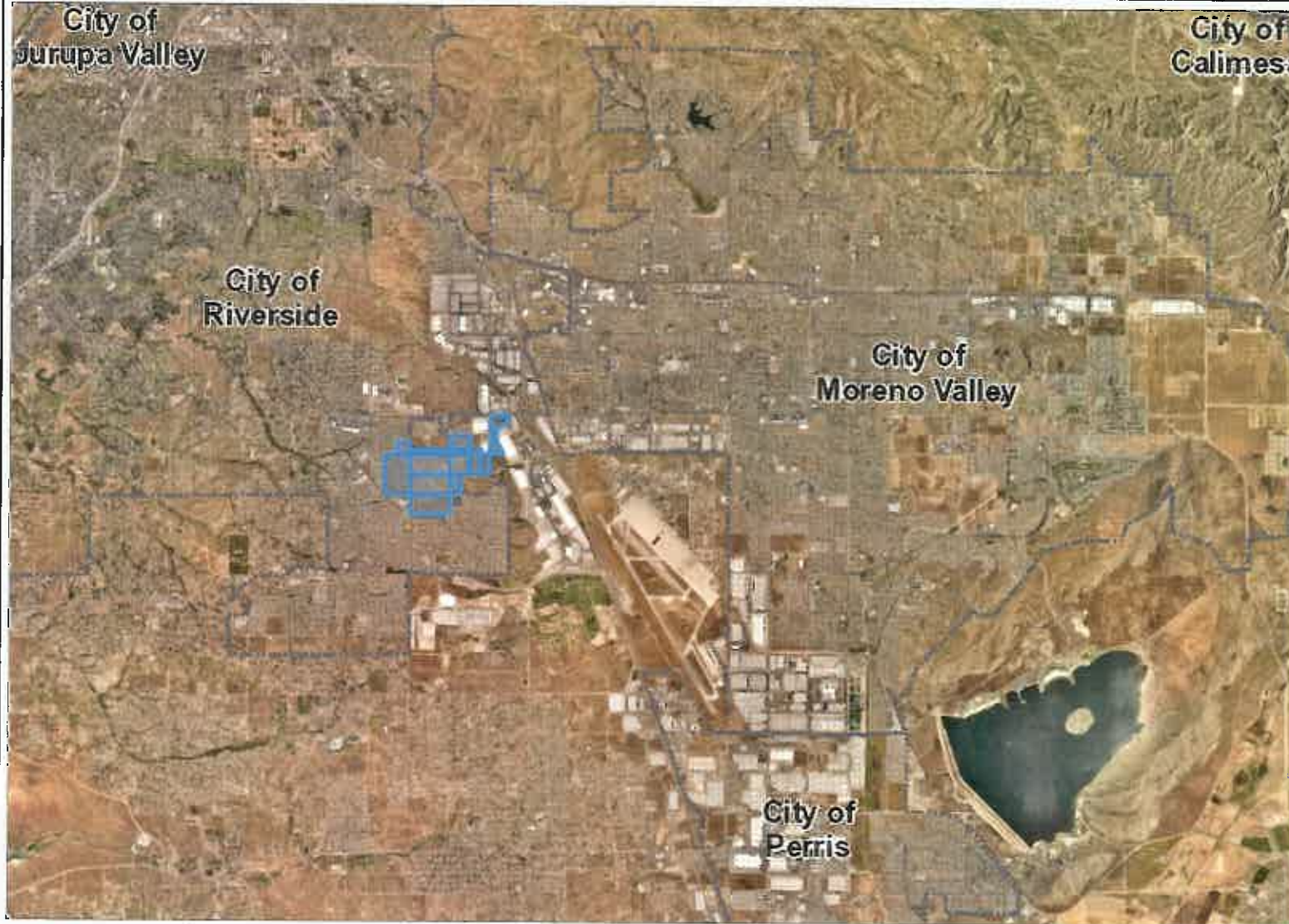


REPORT PRINTED ON... 3/21/2022 3:42:26 PM

© Riverside County GIS

## Notes

# Map My County Map



- Legend**
- City Areas
  - World Street Map



**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.



REPORT PRINTED ON... 3/21/2022 3:56:10 PM

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**Notes**

# Map My County Map



## Legend

-  Blueline Streams
-  City Areas
-  World Street Map



**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.



REPORT PRINTED ON... 3/21/2022 3:48:44 PM

© Riverside County GIS

## Notes

# Map My County Map



- Legend**
- County Centerline Names
  - County Centerlines
  - Blueline Streams
  - City Areas
  - World Street Map



**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

**Notes**



REPORT PRINTED ON... 3/21/2022 3:42:53 PM

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LAND USE LEGEND	
MIXED USE:	42.22 AC
BUSINESS PARK:	65.35 AC
INDUSTRIAL:	143.46 AC
STREETS:	37.7 AC
PUBLIC FACILITIES:	2.84 AC
PARK:	17.86 AC
OPEN SPACE:	67.11 AC
NET DEVELOPABLE:	369.56 AC
PUBLIC FACILITY - EXISTING	2.87 AC
OPEN SPACE - CONSERVATION	445.05 AC
GROSS ACREAGE:	817.90 AC

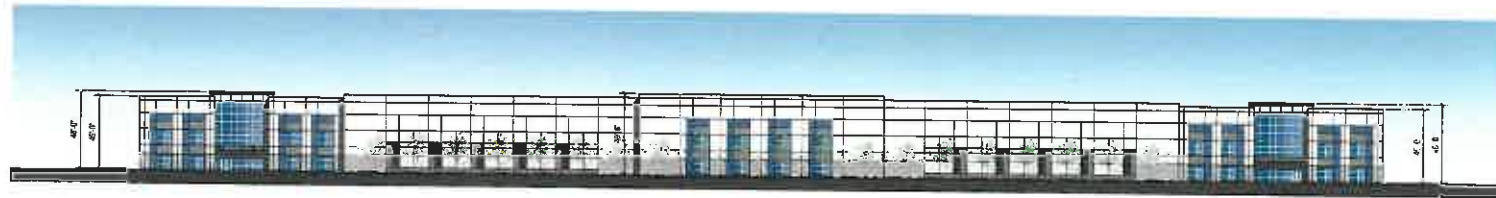
I-215


**MERIDIAN**  
**WEST CAMPUS  
 UPPER PLATEAU**  
 MERIDIAN PARK LLC

**LAND USE  
 PLAN**


RGA PROJECT NO: 2108101  
 DATE: 3/6/22  
 CAD FILE NAME: 2109-03-14  
 DRAWN BY: CS



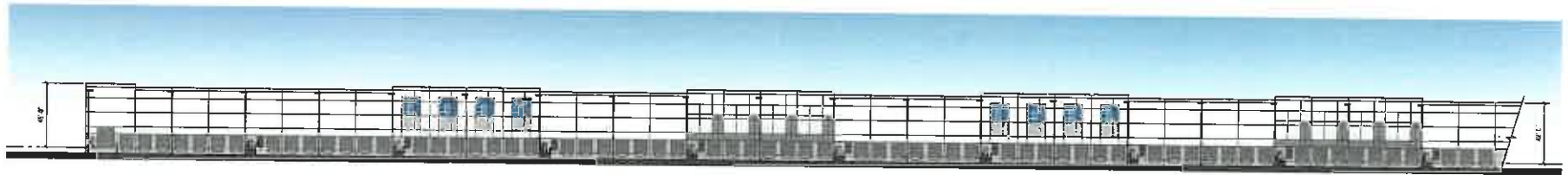


**WEST ELEVATION**  
1" = 30'-0"

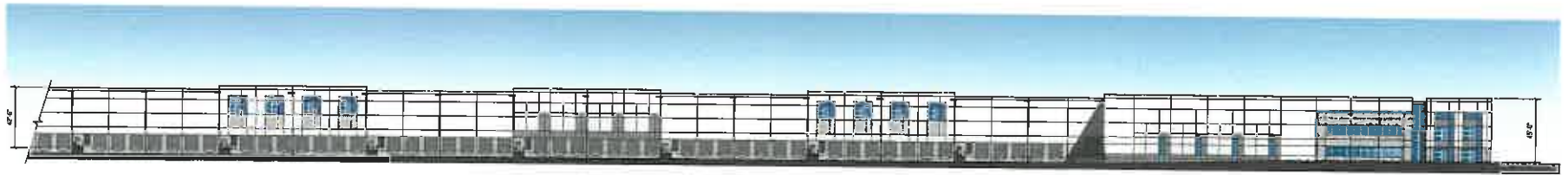
**FINISH SCHEDULE:**

- P-1 PAINTED CONCRETE, FIELD COLOR  
SW 7004 - SNOWBOUND
- P-2 PAINTED CONCRETE, LIGHT ACCENT COLOR  
SW 7016 KANGAROO GRAY
- P-3 PAINTED CONCRETE, DARK ACCENT COLOR  
SW 7042 FAIRSTONE
- P-4 PAINTED CONCRETE, ACCENT COLOR  
SW 7015 GUNTERLY GRAY
- P-5 PAINTED CONCRETE, ACCENT COLOR  
SW 9138 BE TIGER HAWK
- GL-1 GLASS - PRIMARY WINDOW  
PFG SOLARCORE PACIFICA - CLEAR  
ANODIZED ALUMINUM STOREFRONT

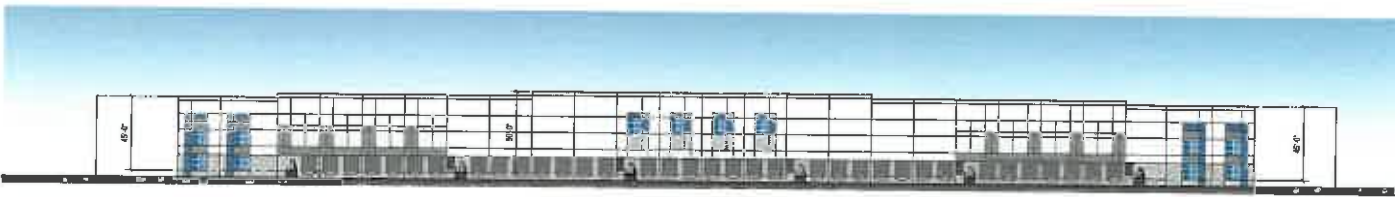
**NOTES:**  
1. ALL ROOFTOP MECH. EQUIPMENT SHALL BE SCREENED FROM VIEW.



**PARTIAL NORTH ELEVATION (SOUTH ELEVATION SIM.)**  
1" = 30'-0"



**PARTIAL NORTH ELEVATION (SOUTH ELEVATION SIM.)**  
1" = 30'-0"



**EAST ELEVATION**  
1" = 30'-0"

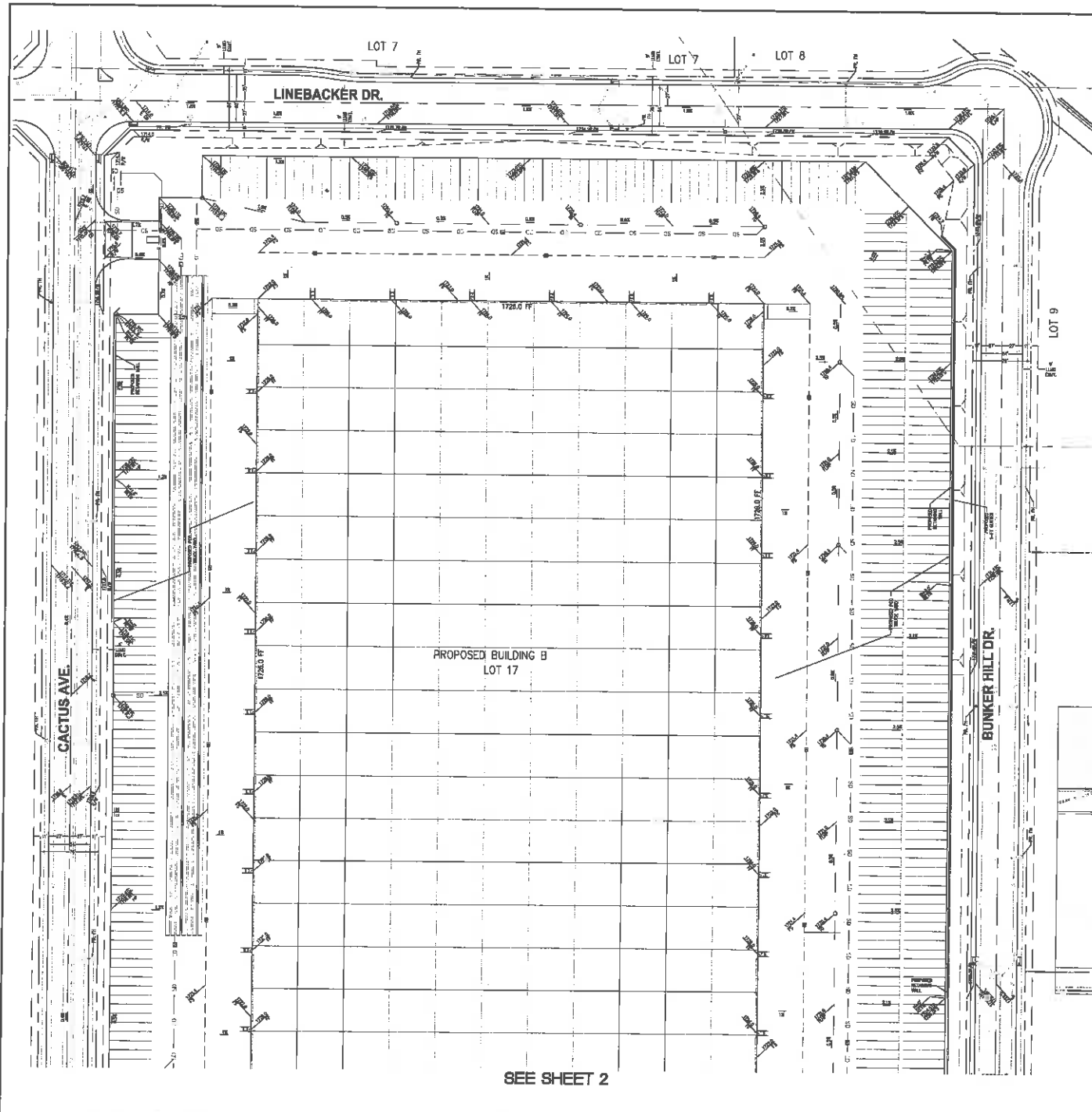


**WEST CAMPUS UPPER PLATEAU - BUILDING B - ELEVATIONS**  
MERIDIAN PARK LLC

NO.	DATE	REVISION/REMARKS	BY	CHKD.

PROJECT NO.	3130100
OWNER NAME	2102-40-44-01
OWNER	CF
DESIGN BY	C
COPYRIGHT	RAYMOND, GUTHRIE & ASSOCIATES, ARCHITECTS
DATE	2012.07.10





**LLMD REQUIREMENTS:**

PRIOR TO SITE GRADING THE GRADING CONTRACTOR SHALL CONTACT THE MARCH IPA LANDSCAPE AND LIGHTING MAINTENANCE DISTRICT (LLMD) MANAGER AT (909) 686-2622 TO COORDINATE ALL ACTIVITY AND IMPACTS UPON LLMD FACILITIES WITHIN THE PUBLIC RIGHT OF WAY AND LANDSCAPE ELEMENTS. THE COORDINATION SHALL ASSURE UNINTERRUPTED OPERATION OF THE LLMD PROTECTION SYSTEM AND SHALL RESOLVE ANY IMPACTS ON LLMD MANHOLES, WALKWAYS, LANTERNS AND IRRIGATION COMPONENTS. INCLUDES OF THE NEED TO WORK ON EXISTING LLMD FACILITIES AS DETERMINED BY THE LLMD MANAGER.

ALL LLMD PROTECTIVE WALKWAY AND IRRIGATION CROSSOVERS BENEATH PAVED AREAS SHALL BE EXISTING OR SCHEDULED AND PVC SLEEVES THREE TIMES THE DIAMETER OF THE PIPE, SLEEVES SHALL BE PLACED AT A DEPTH OF 24" BENEATH FINISHED SURFACE, MEASURING FROM THE TOP OF THE EXISTING CURB. WALKWAYS SHALL BE SCHEDULED SLEEVES WITH A MINIMUM DIAMETER OF 2". WIRE SLEEVES SHALL BE SIZED SO THAT WIRE BUNDLES MAY BE PULLED WITHOUT BENDING ON EACH SIDE OF THE WIRE ASIDE A 1" ROUND HOOD SIDE WITH WIRE CONNECTIONS SHALL BE INSTALLED PRIOR TO CONSTRUCTION SHALL BE USED IN WIRE CONNECTION BOX AND MUST BE BRUSHED ON THE WALKWAY SIDE TO WITH 50% FOR FUTURE DECONSTRUCTION.

ALL CHANGES TO THE LLMD FACILITY AREA SHALL BE COMPLETED IN ACCORDANCE WITH THE APPROVED LLMD LANDSCAPE PLAN AND ANY 1/2" SCHEDULED DRAWING SHALL BE SUBMITTED AND APPROVED UPON COMPLETION AND INSPECTION OF THE WORK. ALL REPAIRS AND/OR INSTALLATIONS IN THE LLMD RIGHT OF WAY AND EXISTING AREAS MUST BE NOTIFIED BY THE LLMD MANAGER.

ALL LLMD IRRIGATION DAMAGED BY CONSTRUCTION ACTIVITIES SHALL BE REPAIRED BY A QUALIFIED IRRIGATION TECHNICIAN WITHIN THIRTY (30) DAYS TO ENSURE LLMD PLANT MATERIAL DOES NOT SUFFER ADDITIONAL DAMAGE OR LOSS OF A 1000 PER PER DAY WILL BE INCURRED UNTIL COMPLETED. A CONTRACTOR SHALL BE NAMED TO THE LLMD CONTRACTOR WITHIN ANY DAMAGE AND WILL BE THE OWNER DIRECTLY AT THE REQUEST OF THE OWNER.

ALL LLMD PLANT MATERIAL DAMAGED SHALL BE REPLACED WITH LIKE SIZED MATERIAL WITHIN THREE (3) BUSINESS DAYS UNLESS AN EXTENSION IS APPROVED IN WRITING BY THE LLMD MANAGER.

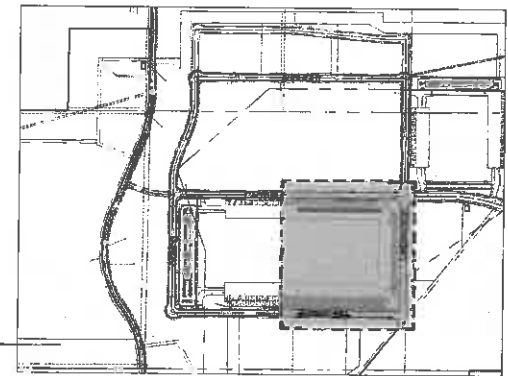
AT NO TIME SHALL ANY CONTRACTOR STORE OR PLACE EQUIPMENT, SIGN, TEMPORARY UTILITIES OR ANY OTHER OBSTRUCTION WITHIN THE PUBLIC RIGHT OF WAY OF LLMD LANDSCAPE ELEMENTS AREAS.

NO CONSTRUCTION PERSONNEL OR SUBCONTRACTORS ANY CONTACT THEIR EQUIPMENT IN THE LLMD AREA. COST FOR DAMAGES SHALL BE AS PER PER DAY.

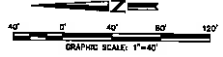
NO OCCUPANCY OF OCCUPANCY WILL BE ISSUED UNTIL ALL LLMD IRRIGATION AND/OR PLANTINGS ARE REPAIRED IN FULL AND TO THE SATISFACTION OF THE LLMD OPERATIONS DIRECTOR AND ANY OUTSTANDING FINES ARE PAID IN FULL.

FROM THE DATE OF ANY LLMD AND EXISTING CONSTRUCTION OR ENCROACHMENT INTO THE LLMD THE LLMD MUST BE NOTIFIED FOR LLMD PRE-INSPECTION AND SCHEDULE OF WORK.

SEE SHEET 2



SHEET INDEX  
SCALE: 1" = 50'



SEE SHEET 2

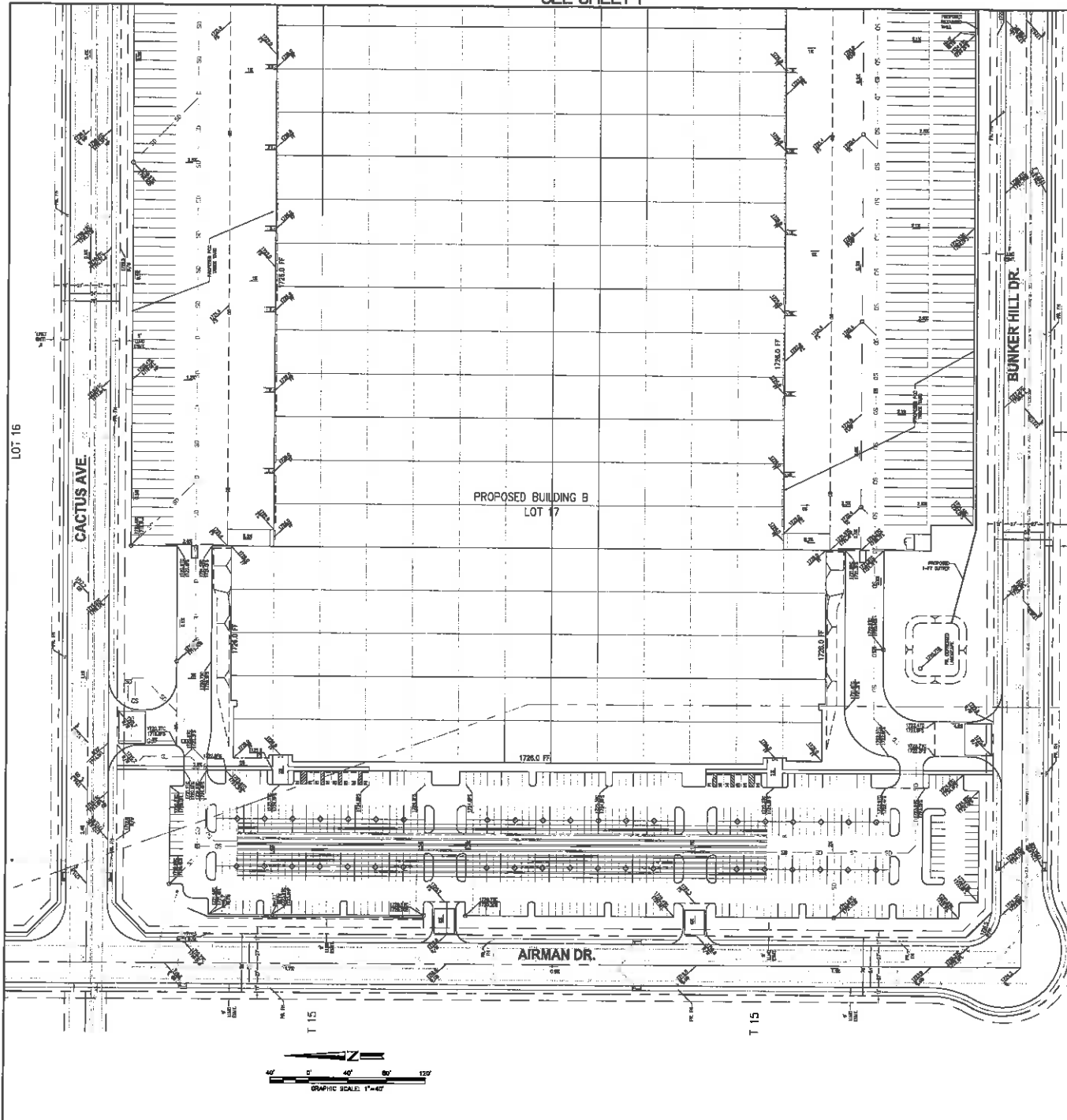
ISS: S. 018 Symply B-04  
Sheet 110  
Anahim Hills, CA 91315  
714-685-6880



NO.	REVISION	DATE

**MERIDIAN - UPPER PLATEAU  
LOT 17 - BUILDING B  
COUNTY OF RIVERSIDE, CALIFORNIA**

PROJECT: CONCEPT  
DATE: 2022-02-02  
DESIGNED BY: DRAWN BY:  
DRAWING FILE:  
PROJECT NO.: B-018B  
SHEET NUMBER:  
**1**  
OF 2 SHEETS  
SCALE: AS SHOWN



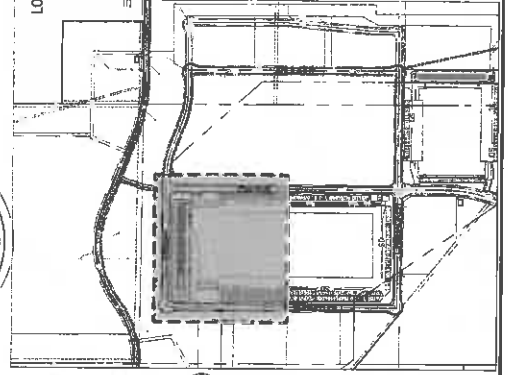
LOT 11

LOT 18

LOT 15

**LLMO REQUIREMENTS:**

- PRIOR TO THE GRADING THE GRADING CONTRACTOR SHALL CONTACT THE MARINA JPA LANDSCAPE AND LIGHTING MAINTENANCE DISTRICT (LMD) MANAGERS AT (562) 866-7000 TO COORDINATE ALL ACTIVITY AND IMPACTS FROM LAND FACILITIES WITHIN THE PUBLIC RIGHT OF WAY AND LANDSCAPE ELEMENTS. THE COORDINATION SHALL ASSURE UNIMPAIRED OPERATION OF THE LAND IRRIGATION SYSTEM AND SHALL CORRECT ANY IMPACTS ON LAND ELEMENTS, TREE LINES AND OPERATING COMPONENTS INCLUDING OF THE NEED TO MOVE OR SLIDE LAND FACILITIES AS DETERMINED BY THE LMD MANAGER.
- ALL LAND IRRIGATION MAIN LINE AND LATERAL OCCUPYING EXISTING PAVED AREAS SHALL BE COACHED IN ACCORDANCE TO PWC SLEEVES THREE THE DIAMETER OF THE PWC SLEEVES SHALL BE PLACED AT A DEPTH OF 3" BENEATH FINISHED SURFACE. MEASURED FROM THE TOP OF THE SLEEVES. CONCRETE SLEEVES SHALL BE SEPARATE SLEEVES WITH A MINIMUM SPACING OF 12" TOWARD VALVE BOX WITH PINE CONNECTIONS SHALL BE INSTALLED. THE CONTRACTOR SHALL BE USED BY PINE CONNECTION BOX AND MUST BE BRANCHED ON THE VALVE BOX LID WITH "T" FOR FUTURE IDENTIFICATION.
- ALL CHANGES TO THE LANDSCAPE AREA SHALL BE COMPLETED AND APPROVED PRIOR TO THE START OF THE WORK. ALL REMOVAL AND/OR INSTALLATIONS IN THE LAND RIGHT OF WAY AND EXISTING AREAS MUST BE APPROVED BY THE LMD MANAGER.
- ALL LAND IRRIGATION DAMAGED BY CONSTRUCTION ACTIVITIES SHALL BE REPAIRED ON A QUALIFIED GRADING CONTRACTOR WITHIN TWENTY FOUR (24) HOURS TO EXISTING LAND PLANT MATERIAL. SOILS MUST BE REPAIRED TO ORIGINAL CONDITION OR BETTER. A SOILS FINE TEST MAY BE REQUIRED UNTIL COMPLETED. SUBSEQUENTLY THE LMD CAN HAVE THEIR LMD CONTRACTOR REPAIR ANY DAMAGE AND OIL THE CURBS DIRECTLY AT THE REQUEST OF THE OWNER.
- ALL LAND PLANT MATERIAL DAMAGED SHALL BE REPLACED WITH LIKE SIZES MINIMUM WITHIN THREE (3) BUSINESS DAYS UNLESS AN EXTENSION IS APPROVED IN WRITING BY THE LMD MANAGER.
- AT NO TIME SHALL ANY CONSTRUCTION SIGN, SIGN EQUIPMENT, SIGNS, TEMPORARY UTILITIES OR ANY OTHER ITEMS WITHIN THE PUBLIC RIGHT OF WAY OR LANDSCAPE ELEMENTS AREAS.
- NO CONTRACTOR PERSONNEL WILL BE ALLOWED ON THE MAPA SUBJECTS AT ANY TIME DURING CONSTRUCTION.
- NO CONSTRUCTION PERSONNEL OR SUBCONTRACTORS MAY CONDUCT THEIR BREAK TIME IN THE LAND AREA. COSTS FOR DAMAGES WILL APPLY AS PER SIGN.
- NO CERTIFICATE OF OCCUPANCY WILL BE ISSUED UNTIL ALL LAND IRRIGATION ANALYSIS PLANTINGS ARE RECEIVED IN-ROAD AND TO THE SATISFACTION OF THE MARINA OPERATIONS MANAGER AND ANY OUTSTANDING FINES ARE PAID IN FULL.
- PRIOR TO ANY WALL AND FENCE CONSTRUCTION OR ENCROACHMENT INTO THE LAND THE LMDA MUST BE NOTIFIED FOR LMD PRE-INSPECTION AND SCHEDULE OF WORK.



**SHEET INDEX**

800 S. Old Springs Road  
 Suite 210  
 Anaheim Hills, CA 92808  
 714-885-6880

**DLR**  
 Design/Construct Engineering, Inc.  
 Civil Engineering and Construction Planning

NO.	REVISION

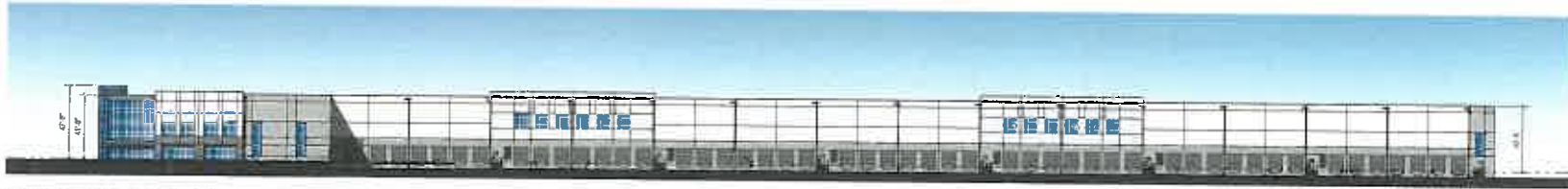
**PROJECT:** MERIDIAN - UPPER PLATEAU  
**LOT 17 - BUILDING B**  
**COUNTY OF RIVERSIDE, CALIFORNIA**

**DATE:** 03/24/24  
**CHECKED BY:** DRW/ML  
**DRAWING FILE:**  
**PROJECT NO.:** 2024-001  
**SHEET NUMBER:**  
**2**  
**OF 2 SHEETS**  
**SCALE:** AS SHOWN

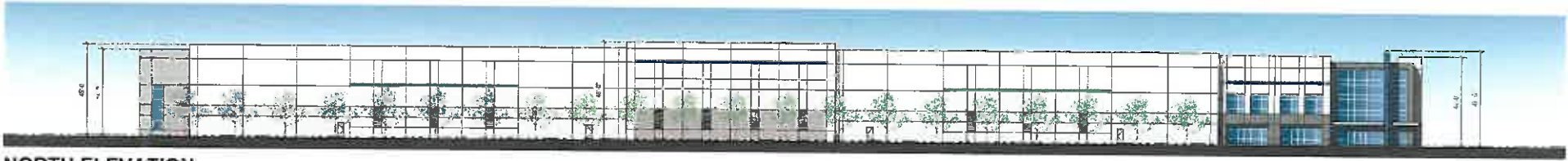




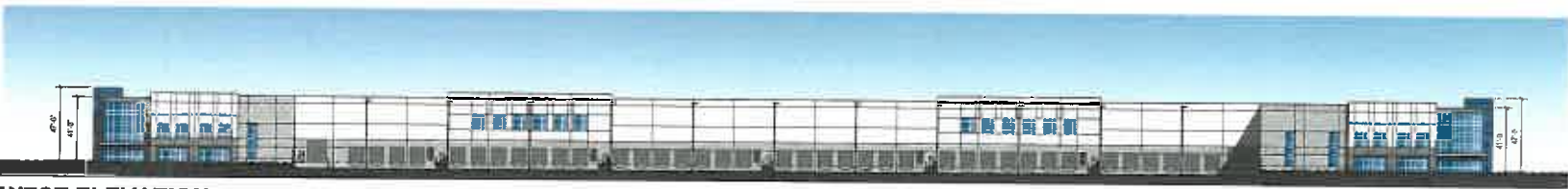
**SOUTH ELEVATION**  
1" = 20'-0"



**EAST ELEVATION**  
1" = 30'-0"



**NORTH ELEVATION**  
1" = 20'-0"



**WEST ELEVATION**  
1" = 30'-0"

**FINISH SCHEDULE:**

- P-1 PAINTED CONCRETE: FIELD COLOR  
SW 7004 - SMOKE/ROUND
- P-2 PAINTED CONCRETE: LIGHT ACCENT COLOR  
SW 7016 MANDFUL GRAY
- P-3 PAINTED CONCRETE: DARK ACCENT COLOR  
SW 7042 PAPERSTONE
- P-4 PAINTED CONCRETE: ACCENT COLOR  
SW 7015 GAINFULITY GRAY
- P-5 PAINTED CONCRETE: ACCENT COLOR  
SW 8178 IN THE BAY
- GL-1 GLASS - FRESANO WINDOW  
PPG SOLARCLOOL PACIFICA - CLEAR  
ANODIZED ALUMINUM STOREFRONT

**NOTES:**  
1. ALL ROOFTOP MECH. EQUIPMENT SHALL BE SCREENED FROM VIEW.

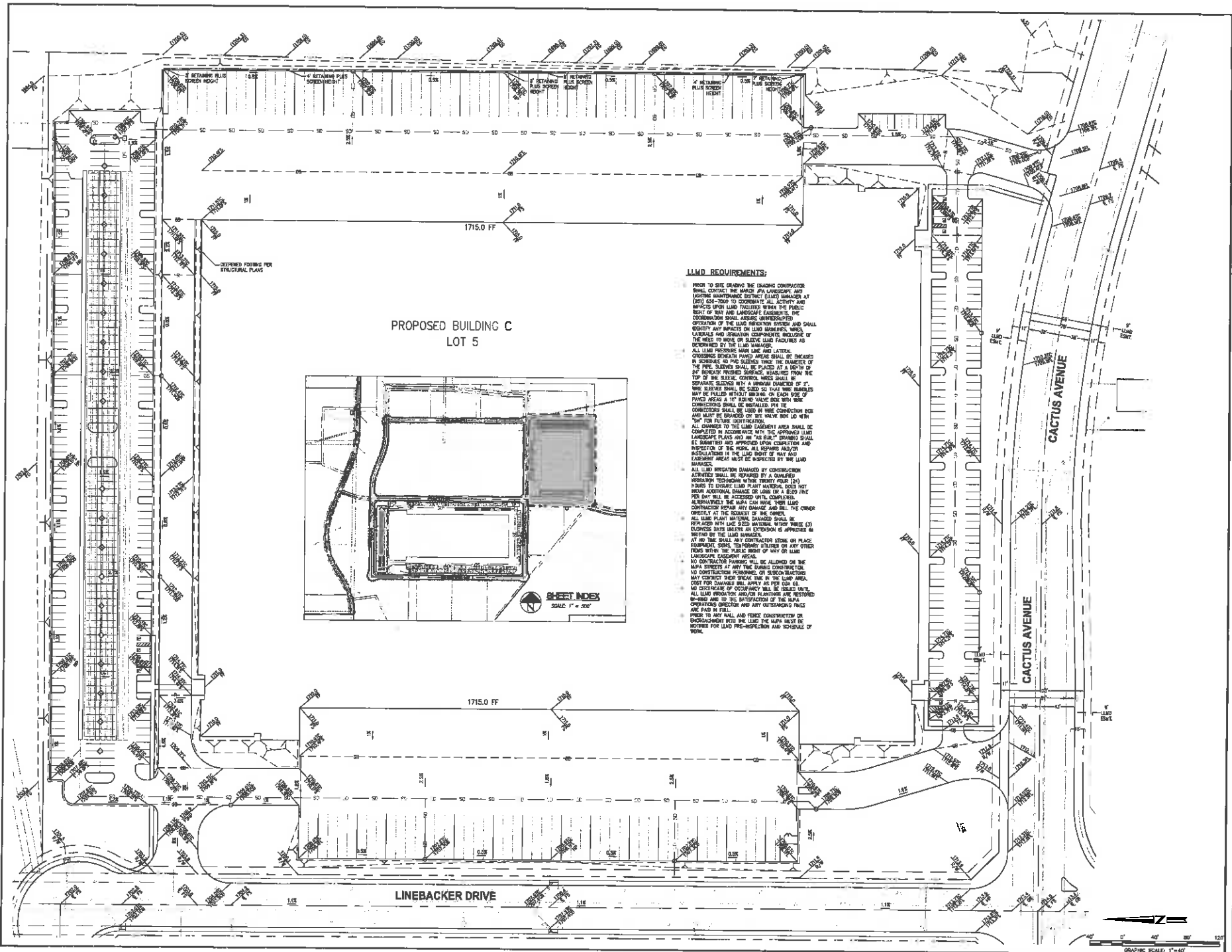


**MERIDIAN**  
**SOUTH CAMPUS - BUILDING C - ELEVATIONS**  
MERIDIAN PARK LLC

NO.	DATE	DESCRIPTION





**LLMO REQUIREMENTS:**

- 1. PRIOR TO SITE GRADING THE GRADING CONTRACTOR SHALL CONTACT THE CITY LANDSCAPE AND LIGHTING MAINTENANCE DISTRICT (LLMO) MANAGER AT (951) 838-7300 TO COORDINATE THE SAFETY AND IMPACTS UPON LLMO FACILITIES WITHIN THE PUBLIC RIGHT OF WAY AND LANDSCAPE ELEMENTS. THE COORDINATION SHALL ASSURE UNDISRUPTED OPERATION OF THE LLMO IRRIGATION SYSTEM AND SHALL VERIFY ANY IMPACTS ON LLMO MANIFOLDS, MAINS, LATERALS AND IRRIGATION COMPONENTS. INCLUDE OF THE NEED TO MOVE OR SLEEVE LLMO FACILITIES AS DESCRIBED BY THE LLMO MANAGER.
- 2. ALL LLMO PRESSURE MAIN LINE AND LATERAL CROSSINGS THROUGH PAVED AREAS SHALL BE BEHIND IN HEIGHT TO THE EXISTING GRADE. THE MANIFOLD OF THE PIPE SLEEVES SHALL BE PLACED AT A MINIMUM OF 24" BELOW FINISHED GRADE. HEADINGS FROM THE TOP OF THE SLEEVE COVERS SHALL BE SEPARATE SLEEVES WITH A MINIMUM CLEARANCE OF 2". WIRE SLEEVES SHALL BE SIZED SO THAT THE MANIFOLD MAY BE CALLED WITHOUT BENDING OR LACKING OF PAVED AREAS A 1" ROUND VALVE BOX WITH WIRE CONNECTIONS SHALL BE INSTALLED AT THE MANIFOLD CONNECTIONS SHALL BE USED IN WIRE CONNECTION BOX AND MUST BE BARRICADED BY 24" HIGH BOX WITH "TOP" FOR FUTURE IDENTIFICATION.
- 3. ALL DAMAGE TO THE LLMO EASEMENT AREA SHALL BE COMPLETED IN ACCORDANCE WITH THE APPROVED LLMO LANDSCAPE PLANS AND THE CITY SHALL BE NOTIFIED BY E-MAIL AND APPROVED UPON COMPLETION AND INSPECTION OF THE WORK. ALL REPAIRS OR INSTALLATIONS IN THE LLMO RIGHT OF WAY AND EASEMENT AREAS MUST BE INDICATED BY THE LLMO MANAGER.
- 4. ALL LLMO IRRIGATION DAMAGED BY CONSTRUCTION ACTIVITIES SHALL BE REPAIRED BY A QUALIFIED IRRIGATION TECHNICIAN WITHIN TWENTY (20) HOURS TO EXHAUST LLMO PLANT MATERIAL DOES NOT REQUIRE ADDITIONAL DAMAGE TO LLMO OF A \$200 FINE PER DAY WILL BE ASSESSED UNTIL COMPLETED.
- 5. AS REQUIRED BY THE CITY CONTRACTOR SHALL REPAIR ANY DAMAGE AND SHALL THE OWNER DIRECTLY AT THE SCENE OF THE DAMAGE.
- 6. ALL LLMO PLANT MATERIAL DAMAGED SHALL BE REPLACED WITH LIKE SIZED MATERIAL WITHIN THREE (3) BUSINESS DAYS UNLESS AN EXTENSION IS APPROVED BY THE LLMO MANAGER.
- 7. AT NO TIME SHALL ANY CONTRACTOR STORE OR PLACE EQUIPMENT OR MATERIALS IN ANY OTHER AREA WITHIN THE PUBLIC RIGHT OF WAY OR LLMO LANDSCAPE EASEMENT AREA.
- 8. NO CONTRACTOR PARKING WILL BE ALLOWED ON THE MAIN STREET AT ANY TIME DURING CONSTRUCTION. NO CONSTRUCTION EQUIPMENT OR SIGNAGE SHALL BE PLACED ON THE MAIN STREET AT ANY TIME. THE COST FOR DAMAGE WILL APPLY AS PER CITY AND COUNTY ORDINANCES.
- 9. ALL LLMO IRRIGATION AND/OR PLUMBING ARE RESTORED BEFORE ANY TO CONSTRUCTION OF ANY OTHER OPERATIONS DIRECTOR AND ANY OUTSTANDING FINES AND FEES MUST BE PAID.
- 10. PRIOR TO ANY WALL AND FENCE CONSTRUCTION OR DEMOLITION WITHIN THE LLMO THE ALFA MUST BE NOTIFIED FOR LAND RECONSTRUCTION AND SCHEDULE OF WORK.

100 S. Old Springs Road  
 Suite 210  
 Anaheim, CA 92708  
 714-253-8850

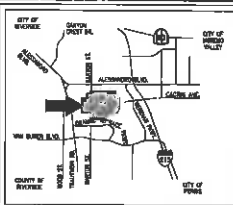
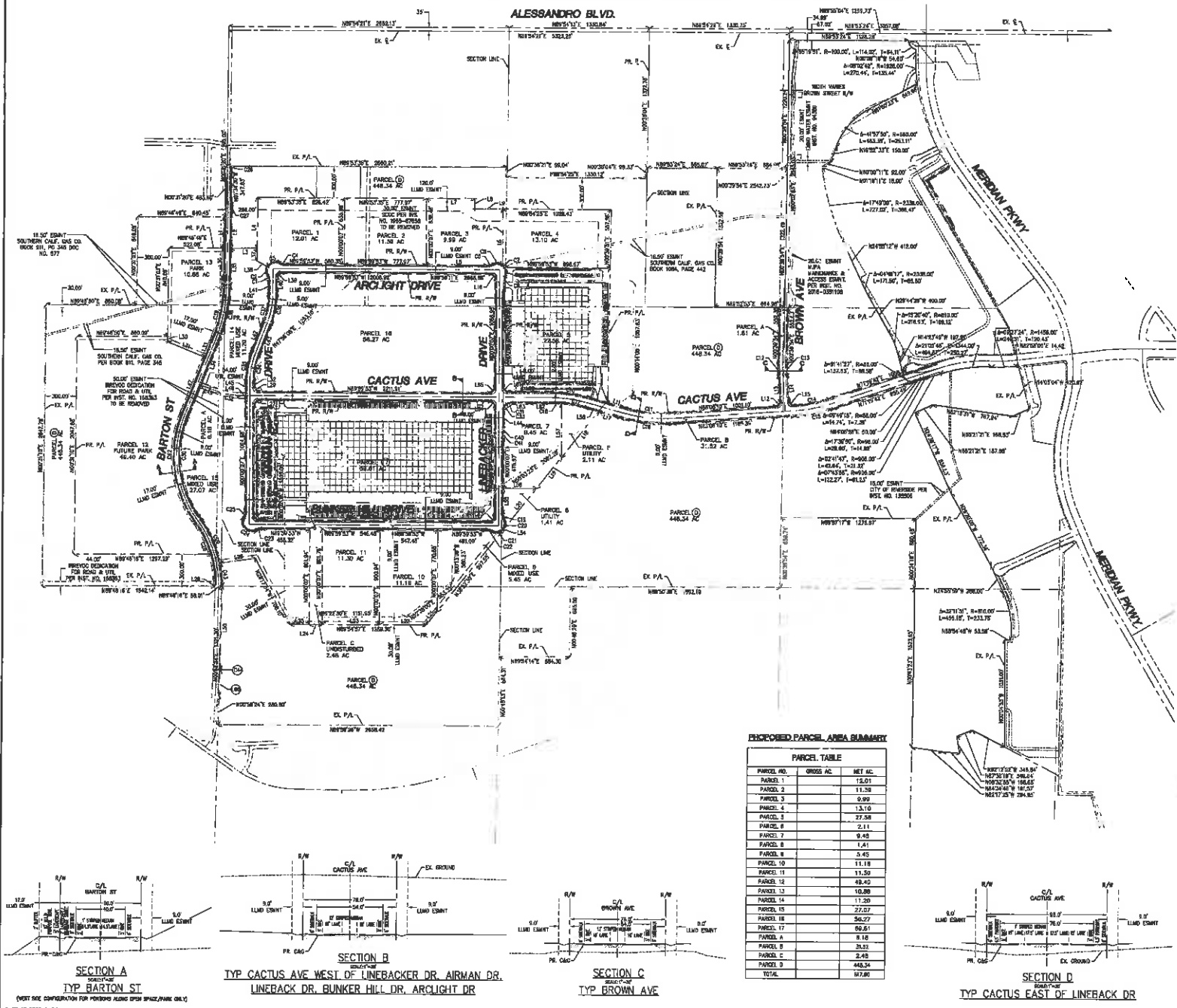
**dlc**  
 Engineering, Inc.  
 Civil Engineering and Construction Services

NO. REVISION	DATE

PROJECT: MERIDIAN - UPPER PLATEAU  
 LOT 5 - BUILDING C  
 COUNTY OF RIVERSIDE, CALIFORNIA

DATE: 08/20/2024  
 DRAWN: J. BROWN  
 CHECKED: M. BROWN  
 PROJECT NO: 20-790  
 SHEET NUMBER: 1  
 OF 1 SHEETS  
 SCALE: AS SHOWN

# TENTATIVE PARCEL MAP No. 38063



**OWNER**  
LEUNG META  
1709 NORTH WAZATIAN AVE  
IRVINE, CA 92714-1873

**DEVELOPER**  
MCDONOUGH P&S, LLC  
C/O BURNETT PROPERTY GROUP  
201 SAN NICOLAS DRIVE, SUITE 270  
COSTA MESA, CA 92626  
CONTACT: MR. JEFF GORDON  
TELEPHONE: (949) 270-8743

**SURVEYOR OF RESPONSIBLE CHARGE**  
DGC ENGINEERING, INC.  
THE SOUTH COAST CENTER ROAD  
ANAHEIM, CA 92805  
TELEPHONE: (714) 408-6800  
DATE:

**SOURCE OF TOPOGRAPHY**  
BY PHOTOGRAMMETRIC METHODS COMPILED & DATED BY DGC ENGINEERING, INC.

**BEYOND MARK**  
A STATIONARY 1/2" X 3/4" U.S. BRASS DISC STAMPED "7 1143 2011" IN A CONCRETE POST ALONG THE E.L.A. PARALLEL SOUTH OF VAN BUREN DRIVE.  
ELEVATION: 1632.70 FEET DATUM: MDD 89

**BASE OF BEARINGS**  
THE BEARING SHOWN HEREON ARE BASED ON THE CENTRALLINE OF ALESSANDRO BLVD. AS SHOWN ON RECORD OF SURVEY AS FILED IN RECORD OF SURVEY BOOK 10, PAGE 20 THROUGH 40, INCLUDING BEING NORTH 89°27'31" EAST.

**LEGAL DESCRIPTION**  
LEGAL DESCRIPTION:  
IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA BEING THAT PORTION OF THAT CERTAIN QUADRANT GRID TO BE MARCH JOHN PETERS' AUTHORITY RECORDED IN THE COUNTY OF RIVERSIDE REC. 2007-03-04-01, OFFICIAL RECORDS OF SAID RIVERSIDE COUNTY, SHOWING A PORTION OF PARCELS 1 AND 2 PER RECORD OF SURVEY 15-11-10, BEING IN BLOCK 10, PHASE 20 THROUGH 40, INCLUDING, OF RECORDS OF SURVEY, SAID COUNTY RECORDS' OFFICE.

**SITE INFORMATION**  
SITE ADDRESS: VICINITY LAND LIND ON THE SOUTHWEST CORNER OF ALESSANDRO BLVD. AND MERIDIAN PARKWAY BEING PORTIONS OF SECTIONS 15, 16, 17, 20, AND 21 OF TOWNSHIP 3 NORTH, RANGE 4 WEST, SAN BERNARDINO MERIDIAN.

**APN**

275-170-007	294-008-001	297-000-002
297-000-008	297-000-001	297-000-006
297-000-002	297-000-005	297-000-007
296-020-002	297-000-004	297-000-003
297-000-005	297-000-003	297-000-004
297-000-001	297-000-004	297-000-001
297-170-036	296-040-001	297-000-003

**EXISTING USE**  
PROPOSED USE:

**NUMBER OF EXISTING LOTS** 1  
**NUMBER OF PROPOSED RESPONSIBLE PARCELS** 17  
**NUMBER OF PROPOSED LITTED PARCELS** 8  
**AREA OF SUSPENSION ROADSIDE** 872.00 ACRES  
**NET AREA OF BUILDABLE PARCELS** 207.80 ACRES

**UTILITIES**  
WATER: WESTERN METROPOLITAN WATER DISTRICT (DM) 780-5000  
SEWER: WESTERN METROPOLITAN WATER DISTRICT (DM) 780-5000  
GAS: RIVERSIDE COUNTY GAS CONTROL DISTRICT (RC) 255-2550  
ELECTRIC: SOUTHERN CALIFORNIA GAS COMPANY (SCG) 457-2550  
TELEPHONE/CABLE: SOUTHERN CALIFORNIA GAS CO. (SCG) 455-4555  
FIREWORK (RSD) 483-0000

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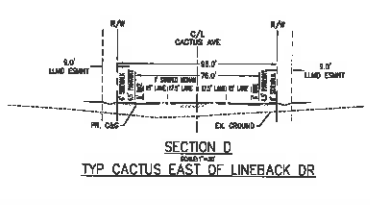
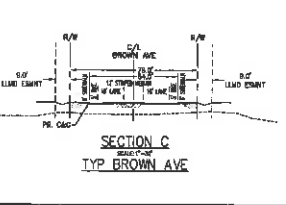
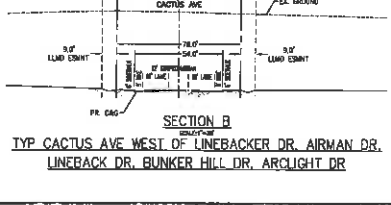
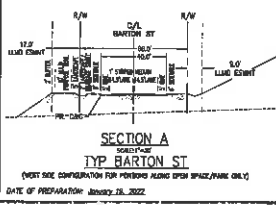
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**ABBREVIATIONS**

- A.P.A. - ASSessor'S PARCEL NUMBER
- C.A. - CURB AND GUTTER
- EX - EXISTING
- PR - PROPOSED
- L - PROPERTY LINE
- R/W - RIGHT OF WAY
- RF - RIGHT OF FENCE
- SE - SANDWICH SERVICE
- SP - SIDEWALK
- TL - TOP OF CURB
- TR - TRUCK
- TY - TYPICAL
- UN - UNDEVELOPED
- W - WATER METERS
- W - WATER MAIN

**PROPOSED PARCEL AREA SUMMARY**

PARCEL NO.	GROSS AC.	NET AC.
PARCEL 1	12.01	
PARCEL 2	11.59	
PARCEL 3	0.99	
PARCEL 4	15.10	
PARCEL 5	27.58	
PARCEL 6	2.11	
PARCEL 7	0.45	
PARCEL 8	1.41	
PARCEL 9	0.45	
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PARCEL 15	27.07	
PARCEL 16	26.27	
PARCEL 17	66.61	
PARCEL 18	8.18	
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PARCEL 20	2.48	
PARCEL 21	448.24	
TOTAL	673.90	



(NOTE SEE CONFORMANCE FOR PERIODS ALONG SPIN SPACES, PARCELS ONLY)  
DATE OF PREPARATION: January 18, 2022



105 S. 34th Street, Suite 210, Costa Mesa, CA 92626  
Tel: 949-441-1888

**PREPARED BY:**  
DGC ENGINEERING, INC.  
Civil Engineering and Surveying License No. 44257

**DATE:**

**PROJECT:**  
MERIDIAN UPPER PLATEAU  
SW COR OF ALESSANDRO BLVD & MERIDIAN PKWY  
RIVERSIDE, CALIFORNIA

**SHEET:** 174  
**DATE:** 1/18/22  
DRAWN BY: SHANE A. BURNETT  
CHECKED BY: JEFF GORDON  
PROJECT NO.: 20-783  
SHEET NO.: 80-100  
OF 2 SHEETS  
SCALE: AS SHOWN

**PARCEL**  
**SW CORN OF ALESSANDRO BLVD & MERIDIAN PKWY**  
**RIVERSIDE, CALIFORNIA**  
**TENTATIVE PARCEL MAP**



JEFFREY A. BERMAN  
 ENGINEER  
 11-00-00000  
 11-00-00000  
 11-00-00000

DATE: 08/15/2011 10:58:11 AM  
 PROJECT: 11-00-00000  
 SHEET: 2  
 TOTAL SHEETS: 2



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# WEST CAMPUS UPPER PLATEAU

## SPECIFIC PLAN NO. XXX

---

*Prepared for:*  
**MARCH JOINT POWERS AUTHORITY**

*Developed by:*  
**MERIDIAN PARK WEST, LLC**

*Prepared by:*  
**T&B PLANNING, INC.**



# WEST CAMPUS UPPER PLATEAU

## SPECIFIC PLAN NO. XXXX

*Prepared for:*

**MARCH JOINT POWERS AUTHORITY**  
14205 Meridian Parkway, Suite 140  
Riverside, CA 92518  
(951) 656-7000

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JOB NUMBER: 1076-008

FIRST SCREENCHECK: FEBRUARY 9, 2022

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## ES EXECUTIVE SUMMARY

### ES.1 PROJECT OVERVIEW

The West Campus Upper Plateau Specific Plan covers approximately 817.9 acres in the northwestern portion of the March Joint Powers Authority (MJPA). The Specific Plan property is located south of Alessandro Boulevard, west of Meridian Parkway, north of Grove Community Drive, and east of Trautwein Road. The City of Riverside surrounds the northern, western, and southern ends of the Specific Plan Area, along with some small County of Riverside "islands" north of the Specific Plan Area.

ON September 12, 2012, a Settlement Agreement was entered between and among the Center for Biological Diversity (CBD), the San Bernardino Valley Audubon Society, MJPA, and LNR Riverside LLC as the complete settlement of the claims and actions raised in *Center for Biological Diversity v. Jim Bartel, et al.* (CBD Settlement Agreement, MJPA 2012). The CBD Settlement Agreement contemplated the division of western acreage under the jurisdiction of the MJPA, including the Project site, into a Conservation Area, Developable Area, Proposed Park Area and Water Quality/Open Space Area.

The location of the West Campus Upper Plateau in regional and local contexts is depicted in *Figure ES-1, Regional Location Map*, which shows the relationship of the Specific Plan property with nearby cities, counties, and unincorporated communities. *Figure ES-2, Local Vicinity Map*, depicts the surrounding land use of the Specific Plan Area.

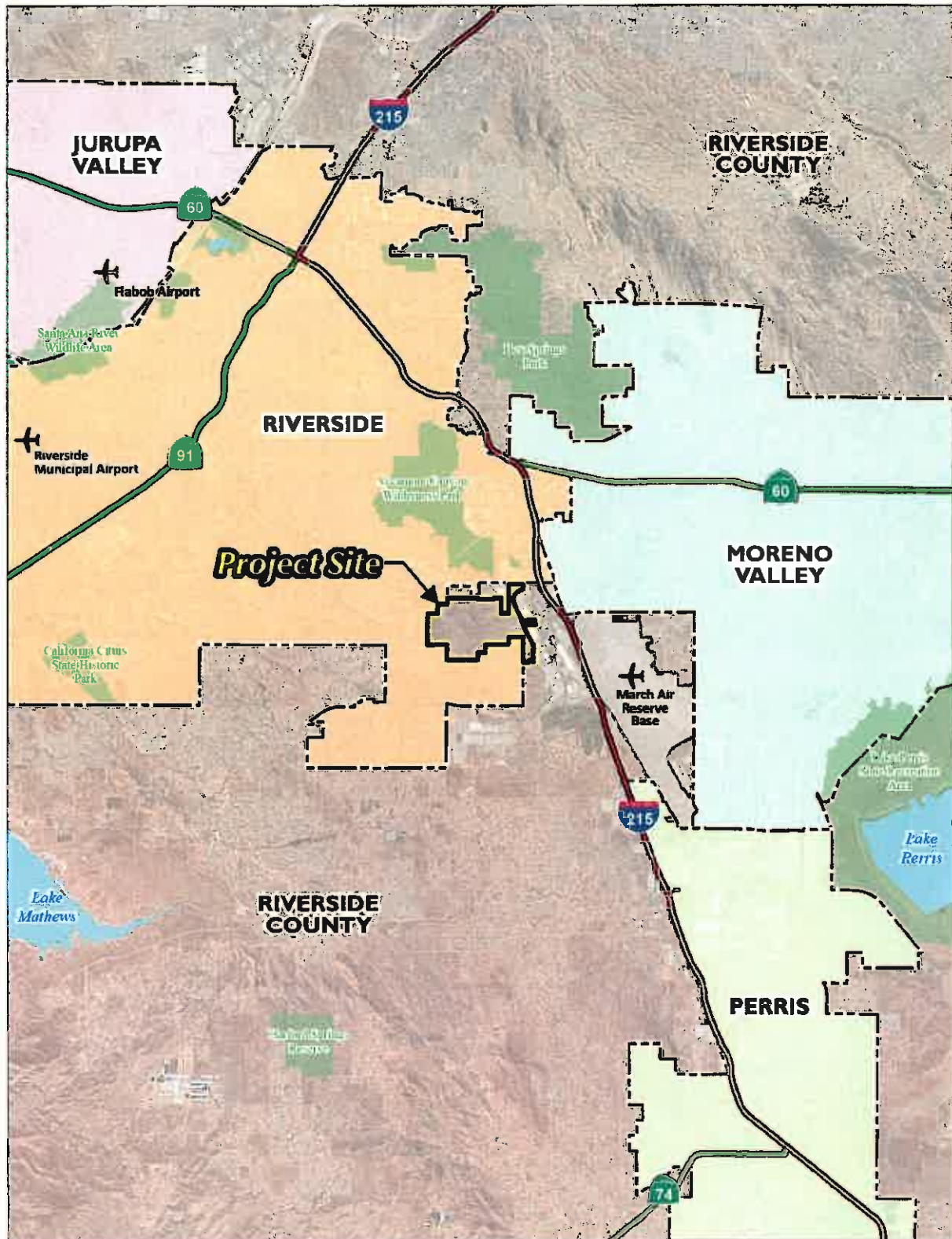
The information contained in this Specific Plan provides guidance for a development accommodating Business Park, Industrial, Mixed Used, Public Facilities, and Open Space land uses. The West Campus Upper Plateau Specific Plan is envisioned to contain industrial, business park, and non-residential mixed-use buildings supported by public roads and utility infrastructure systems, private driveways, parking lots, truck courts, lighting, landscaping, signage, and other functional and decorative features. Hiking and biking trails are provided within the Open Space surrounding the proposed development area to encourage recreational activities by surrounding residents, employees, and visitors.

The Business Park, Industrial, and Mixed Used land uses are surrounded by Open Space areas that provide a minimum 300' buffer for the nearby residents in the City of Riverside and County of Riverside. As designed, building users are expected to be a mixture of businesses that bring job opportunities and economic growth to the MJPA and the surrounding cities.

The following land use types specified in the MJPA General Plan would be developed as part of the West Campus Upper Plateau project:

- **Business Park (BP):** including administrative, financial, light manufacturing, and commercial services.
- **Industrial (IND):** including manufacturing, warehousing, e-commerce and associated uses.
- **Mixed Use (MU):** complementary uses, including commercial retail, office, research and development, industrial and others.

- **Park/Recreation/Open Space (P/R/OS):** primarily passive open spaces and recreational areas.
- **Public Facilities (PF):** range of public, quasi-public, and private uses such as public cultural and historical facilities, government facilities, public utilities, and major roads.

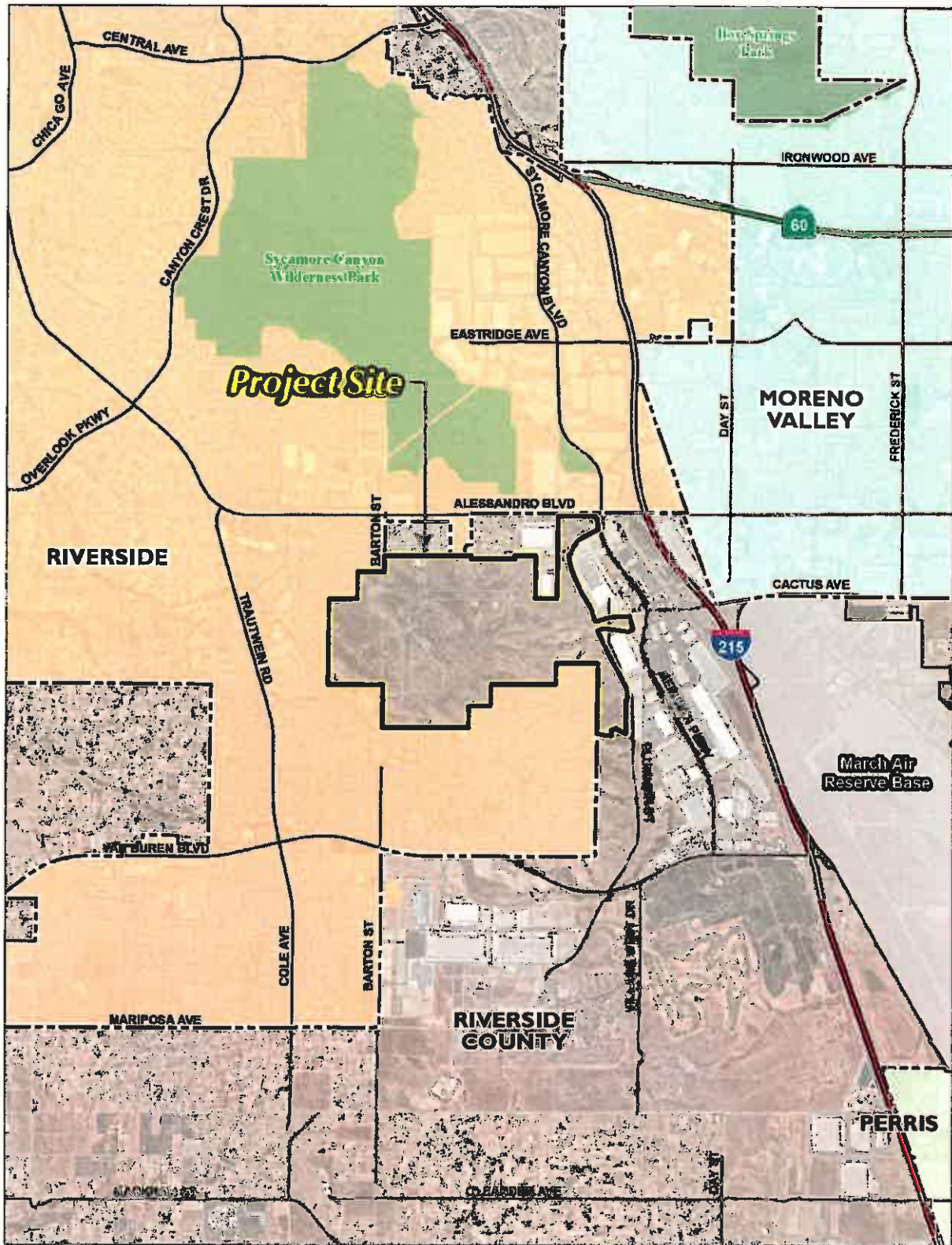


Source(s): ESRI, Nearmap (2021), RCTMLA (2021)

Figure ES-1



Regional Location Map



Source(s): ESRI, Nemap (2021), RCTMLA (2021)

Figure ES-2



Local Vicinity Map

## ES.2 OTHER GOVERNING DOCUMENTS

In addition to this Specific Plan, which includes a Land Use Plan, Infrastructure Plan, Development Regulations, Design Guidelines, and Implementation Plan, the following documents also contain applicable information relevant to the project site:

- March Joint Powers Authority General Plan. Established in 1999, the General Plan includes goals and polices pertaining to land use, transportation, noise/air quality, housing, resource management, and safety/risk management that pertain to approximately 4,400 acres of land administered by the March Joint Powers Authority.
- March Joint Powers Authority Development Code. These regulatory provisions govern over topics on which this Specific Plan's development regulations do not address. Note: where the requirements of this Specific Plan differ from the requirements of the MJPA Development Code, this Specific Plan takes precedence.
- Air Installations Compatible Use Zone Study and Airport Land Use Compatibility Plan for the March Air Reserve Base. The Air Installations Compatible Use Zone Study was finalized in 2018. This document provides a land use compatibility analysis resulting in a number of policies and guidelines intended to ensure the continued operation of the March Air Reserve Base while minimizing hazards and impacts to the built environment and future development surround the Base.
- Mitigation Monitoring and Reporting Program (MMRP). An Environmental Impact Report (EIR) was prepared in compliance with the California Environmental Quality Act (CEQA) for the West Campus Upper Plateau Specific Plan. The MMRP stipulates measures required to be implemented to mitigate the environmental effects associated with the future development represented in the Specific Plan Area.

## ES.3 SPECIFIC PLAN COMPONENTS

The West Campus Upper Plateau Specific Plan is organized into the following chapters.

### **Chapter 1 – Introduction:**

Describes the purpose and objectives of this Specific Plan, related entitlement approvals for implementing development, and the general relationship between this Specific Plan and the March Joint Powers Authority General Plan.

### **Chapter 2 – Land Use:**

Describes the West Campus Upper Plateau's development plan, which includes Industrial, Business Park, Mixed Use, Public Facility, Park, Open Space, and Open Space – Conservation land uses. This chapter also provides information on the open space areas identified in the Specific Plan, which constitute the majority of acreage within the project area.

### **Chapter 3 – Development Regulations:**

Provides information on various applicable development regulations in the MJPA, including permitted, conditional and ancillary land use of the West Campus Upper Plateau. This chapter also includes development standards for the Specific Plan area.



**Chapter 4 – Design Guidelines:**

Provides the site planning, landscaping, and architectural theme within the West Campus Upper Plateau Specific Plan. This chapter provides guidelines on architectural design, landscape design, streetscapes, walls and fencing, and signage.

**Chapter 5 – Transportation:**

Describes the overall circulation and street network proposed to serve the Specific Plan, including street cross sections and integration/connection with the abutting existing road system.

**Chapter 6 – Infrastructure and Grading:**

Provides information on the planned backbone water, sewer, and storm drain systems; the planned dry utility network; and the preliminary grading concept for the development of the Specific Plan.

**Chapter 7 – Implementation:**

Provides the policies and procedures for the MJPA's review and approval of implementing projects within the West Campus Upper Plateau Specific Plan. This chapter describes the methods and procedures for interpreting and amending the Specific Plan, as necessary. A summary of maintenance responsibilities is also identified in this Chapter.

**Chapter 8 – Consistency with the General Plan:**

Include a matrix evaluating the consistency of the West Campus Upper Plateau Specific Plan to each of the applicable policies of the MJPA General Plan.

# 1 INTRODUCTION

## 1.1. SPECIFIC PLAN PURPOSE

The purpose of this Specific Plan is to guide and direct the development of the subject project site into a master-planned industrial park, known as the West Campus Upper Plateau. The site is located within the western portion of the March Joint Powers Authority (MJPA) jurisdiction, more specifically within the West March Planning Subarea, west of the current terminus of Cactus Avenue. Projects proposed for development within the boundaries of the West Campus Upper Plateau Specific Plan are required to demonstrate substantial conformity with the standards and information contained in this Specific Plan.

Situated near Interstate 215 with access to two additional major freeways, development within the West Campus Upper Plateau is poised to successfully accommodate users who rely upon access and close proximity to the local and regional transportation network. The Specific Plan area is located less than one mile west of Interstate 215, less than 3 miles southwest of State Route 60, and approximately 6 miles southeast of State Route 91. Proximity to these routes provide not only the ability to quickly receive material and move goods but also provide ease of workforce access. *Figure 1-1, Aerial Photograph*, depicts the surrounding land use of the Specific Plan area.

## 1.2. SPECIFIC PLAN OBJECTIVES

This Specific Plan achieves the following objectives:

- Provide a land use plan for the development of a state-of-the-art commerce area that accommodates modern business and industrial activities.
- Attract and sustain industrial, business park and mixed uses within the Specific Plan area that are buffered from sensitive uses by ample open space and landscape.
- Locate businesses that rely on transportation efficiency in an area of the MJPA that offers convenient access to the state highway system.
- Provide opportunities for positive economic benefit to the MJPA and region, including new net revenues which can be used for vital services.
- Diversify the MJPA's range of employment-generating land uses.
- Provide opportunities for the development and operation of active and passive use parks and trails that take advantage of and embrace the location.
- Identify capital improvements for water, recycled water, sewer, storm drain, and circulation facilities that serve planned land uses within and adjacent to the Specific Plan area.
- Define guidelines and standards for architecture, landscaping, entry monuments/signage, and walls and fencing within the Specific Plan area.
- Set forth a development phasing sequence that is aligned with a logical sequence for the installation of supporting on-site and off-site infrastructure.
- Implement the terms and conditions agree upon in the September 12, 2012, Settlement Agreement entered into between and among the CBD, the San Bernardino Audubon Society, MJPA, and LNR Riverside LLC, as the complete settlement of the claims and actions raised in *Center for Biological Diversity v. Jim Bartel, et al.*
- Implement the Conservation Area as a means of environmental protection.



Source(s): ESRI, Nearmap (2021), RCTMLA (2021)

Figure 1-1



Aerial Photograph

### 1.3. AUTHORITY

This Specific Plan is a regulatory document prepared pursuant to the provisions of California Government Code §§ 65450 through 65457, which grants local government agencies the authority to prepare Specific Plans for the systematic implementation of their General Plan for all or part of the area covered by the General Plan. While the March Joint Powers Authority General Plan covers over 4,400 acres, this Specific Plan concentrates on the future development of the approximately 807.54-acre West Campus Upper Plateau property.

California Government Code §§ 65450 through 65457 establish the authority to adopt a Specific Plan, identify the required contents of a Specific Plan, and mandate consistency with the General Plan. According to California Government Code § 65451:

- (a) A Specific Plan shall include text and a diagram which specify all the following in detail:
  - (1) The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan.
  - (2) The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the plan.
  - (3) Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable.
  - (4) A program of implementation measures including regulations, programs, public works projects, and financing measures, necessary to carry out items (1), (2), and (3).
- (b) The Specific Plan shall include a statement of the relationship of the Specific Plan to the General Plan.

This Specific Plan includes each of the required elements listed above and establishes the essential link between the policies of the March Joint Power Authority General Plan and the West Campus Upper Plateau property. All future development plans and implementing construction activities within this Specific Plan are required to be consistent with the requirements set forth in this Specific Plan and with all other applicable City regulations.

### 1.4. BACKGROUND AND HISTORY

Since 1988, the federal government closed and realigned military bases throughout the United States. In order to limit the economic disruption caused by base closures, the California State Legislature authorized the formation of joint powers authorities to regulate the redevelopment of closed/realigned military installations. Joint powers authorities are empowered to activate a redevelopment agency for each base to be closed. In 1993, the federal government, through the Defense Base Closure and

Realignment Commission, called for the realignment of March Air Force Base (MAFB) and for a substantial reduction in its military use. In April 1996, MAFB was re-designed as an Air Reserve Base (ARB). The cities of Moreno Valley, Perris, and Riverside, and the County of Riverside formed the



March Joint Powers Authority (MJPA), which continues to serve as the reuse authority for the over 4,400 acres of declared surplus property. The MJPA prepared several planning, policy, and regulatory documents to guide the redevelopment of the former MAFB. These documents include:

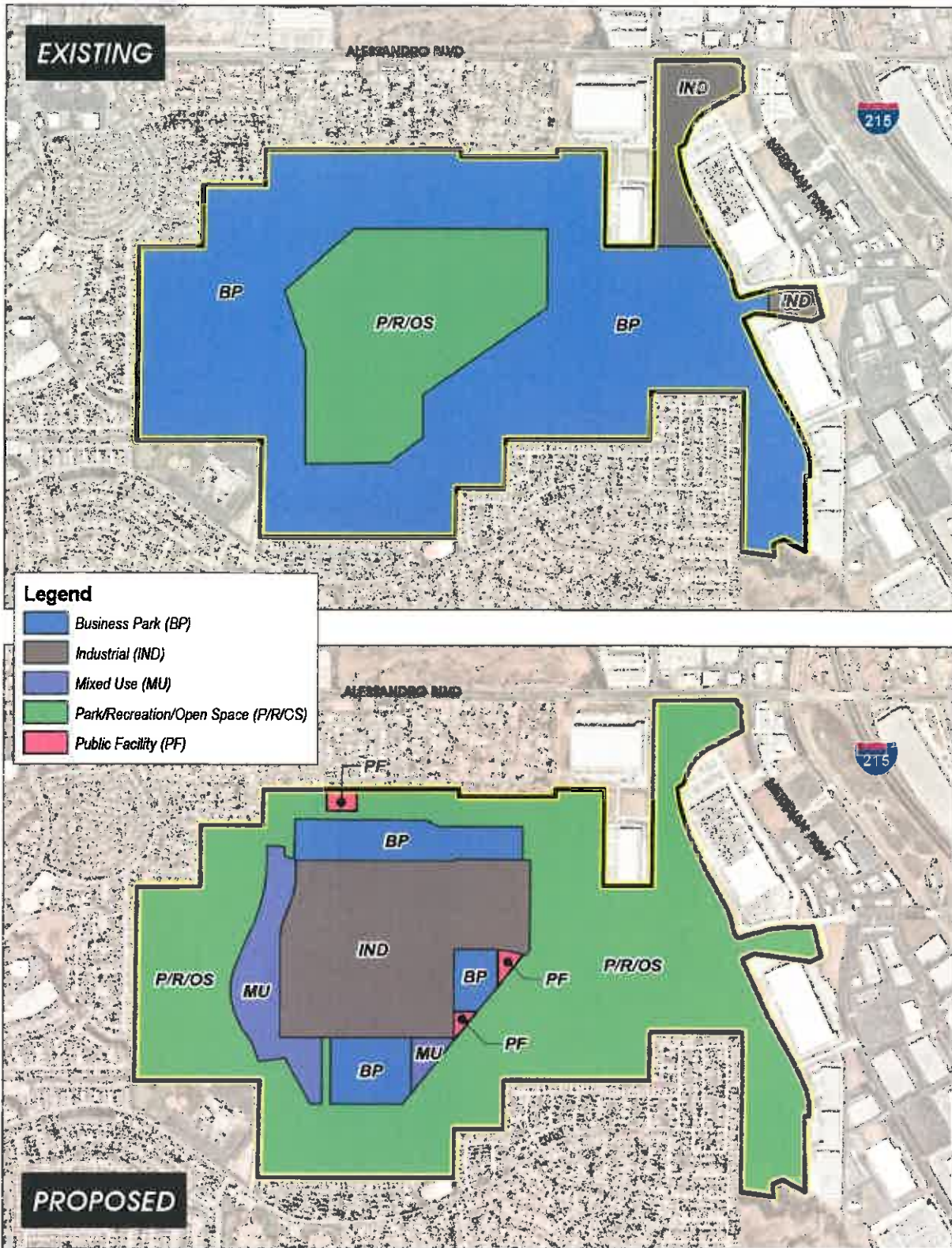
- March Air Force Base Master Reuse Plan, MJPA (November 1995)
- Final Environmental Impact Statement: Disposal of Portions of March Air Force Base (February 1996)
- Final Environmental Impact Report for the March Air Force Base Redevelopment Project (June 1996)
- Redevelopment Plan for the March Air Force Base Redevelopment Project (June 1996)
- March Joint Powers Authority Development Code (July 1997)
- General Plan for the March Joint Powers Authority (September 1999)
- Master Environmental Impact Report for the General Plan of the March Joint Powers Authority (September 1999)
- March Business Center Statutory Development Agreement (2003)
- MJPA General Plan Amendment (February 2003)
- March Business Center Design Guidelines (2003)
- Final Air Installations Compatible Use Zone Study, March Air Reserve Base (2018)

On September 12, 2012, a Settlement Agreement was entered between and among the Center for Biological Diversity (CBD), the San Bernardino Valley Audubon Society, MJPA, and LNR Riverside LLC as the complete settlement of the claims and actions raised in *Center for Biological Diversity v. Jim Bartel, et al.* (CBD Settlement Agreement, MJPA 2012). The CBD Settlement Agreement contemplated the division of the West Campus Upper Plateau Specific Plan Area into a Conservation Area, Developable Area, Proposed Park Area and Water Quality/Open Space Area. The Specific Plan land use plan contain herein is designed representative of and to be consistent with this Settlement Agreement.

## 1.5. PLANNING CONTEXT

Figure 1-2, *Existing & Proposed General Plan Land Uses*, depicts the current and proposed General Plan land use designations in the West Campus Upper Plateau Specific Plan Area. Figure 1-3, *Existing and Proposed Zoning*, depict the existing and proposed zoning for the West Campus Upper Plateau.



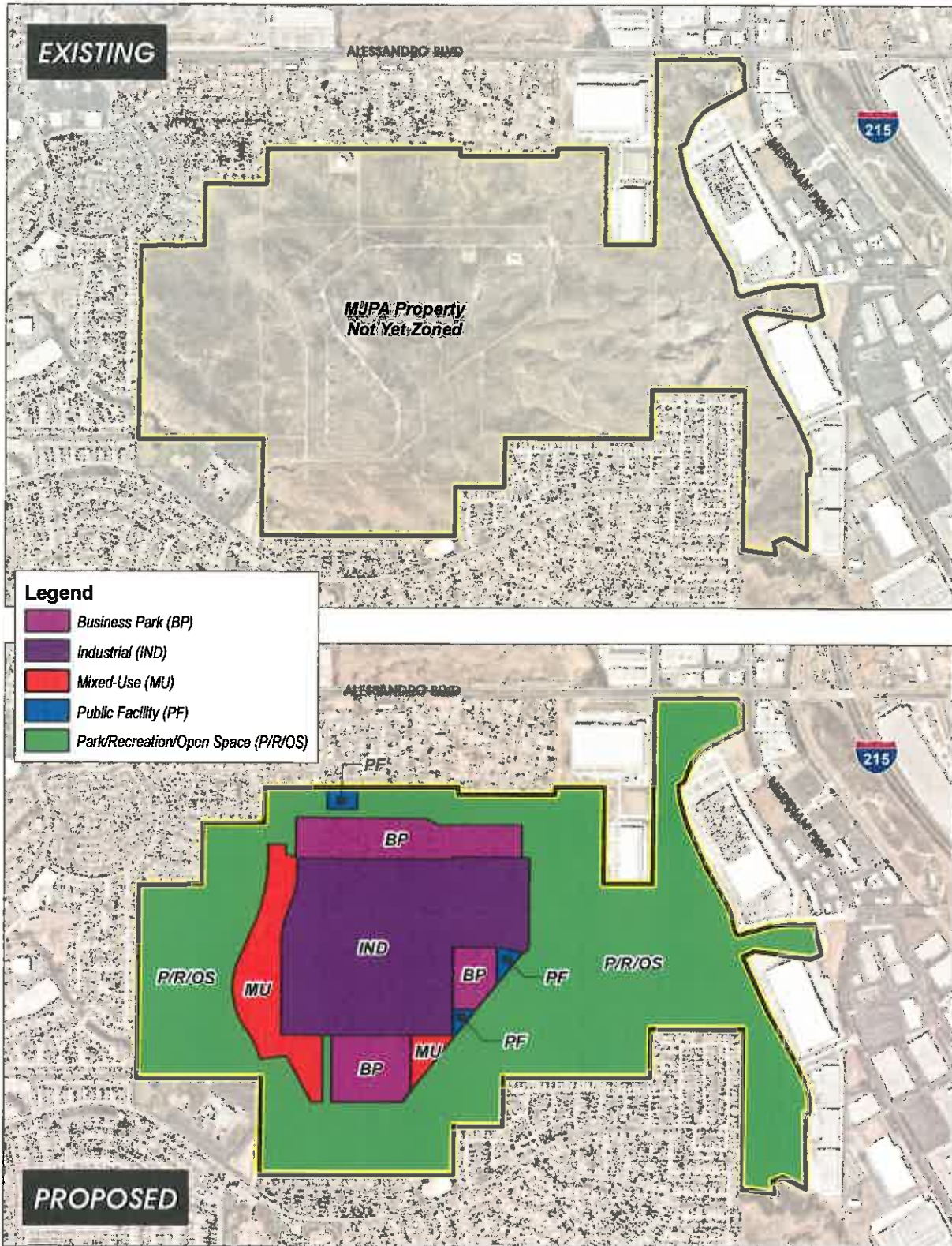


Source(s): ESRI, March JPA General Plan (2017), Nearmap (2021)

Figure 1-2



Existing & Proposed General Plan Land Uses



Source(s): ESRI, March JPA (2021), Nearmap (2021)

Figure 1-3



Existing & Proposed Zoning

## 1.7. ENVIRONMENTAL IMPACT REPORT

An EIR was prepared (SCH No. 2021110304) in accordance with the provisions under CEQA to evaluate and disclose the potential environmental consequences of the West Campus Upper Plateau Specific. The EIR serves as a project-wide environmental document for the West Campus Upper Plateau Specific Plan Area. The March Joint Powers Authority (MJPA) serves as the lead agency in the preparation and certification of the EIR. The West Campus Upper Plateau Specific Plan and EIR jointly serve a path to develop the Specific Plan Area as intended, taking into account applicable policies, goals, objectives, and environmental considerations of the MJPA General Plan.



## 1.8. DISCRETIONARY ACTIONS

The following discretionary actions will be required as part of the West Campus Upper Plateau Specific Plan:

- **GENERAL PLAN AMENDMENT:** A General Plan Amendment to the land use plan described herein is necessary to reflect the changes to land uses and ultimate roadway configuration as represented in *Figure 1-2, Existing & Proposed General Plan Land Uses*.
- **SPECIFIC PLAN:** The West Campus Upper Plateau Specific Plan requires review by and approval of the MJPA. Once adopted, this Specific plan will create a comprehensive land use document that identifies and defines land uses within the Specific Plan.
- **CHANGE OF ZONE:** Under the existing MJPA Zoning Map, the West Campus Upper Plateau Specific Plan Project Area did not have zoning designations identified. As a result of the West Campus Upper Plateau Specific Plan, the MJPA will establish zoning within the Specific Plan Area that is consistent with the land uses and locations identified within the Specific Plan.
- **TENTATIVE TRACT MAP:** A Tentative Tract Map will be approved by the MJPA for the Specific Plan Area indicating the approximate boundaries and dimensions of parcels and streets. Following the Tentative Tract Map, a Final Map will become the legal document that identifies the developable parcels within the Specific Plan.
- **PLOT PLANS:** All development within the Specific Plan property shall be subjected to a Plot Plan review. Adoption of this Specific Plan by the MJPA includes the design guidelines contained in *Chapter 4, Design Guidelines*, which shall be the design criteria by which development projects with the Specific Plan shall be reviewed during the Plot Plan review.
- **DEVELOPMENT AGREEMENT:** Due to the scale and complexity of the Project, a Development Agreement is proposed to vest the Project entitlements and fees, ensure financing of public improvements required by the conditions of approval, and provide certain Community Benefits including compliance with the terms of the 2012 Settlement Agreement, and provision of new public benefits, including, but not limited to, expansion of employment opportunities for area residents.



## 2 LAND USE

### 2.1. LAND USE OVERVIEW

This chapter identifies the types of land uses to be allowed in the Specific Plan Area and provides regulations and standards to govern future development. In accordance with the General Plan, this Specific Plan accommodates land uses that support future growth and development in the area. The West Campus Upper Plateau Specific Plan Land Use provisions reference the following policies, regulations, and guidelines:

- MJPA General Plan (1999)
- MJPA Development Code (1997)

This chapter specifies broad land use categories that will guide the development of the Specific Plan Area. Within each broad category, specific land uses are identified, together with an indication of whether such uses are permitted, subject to a conditional use permit, or not allowed. In addition, development regulations that will govern the development of the individual projects comprising the West Campus Upper Plateau are described.

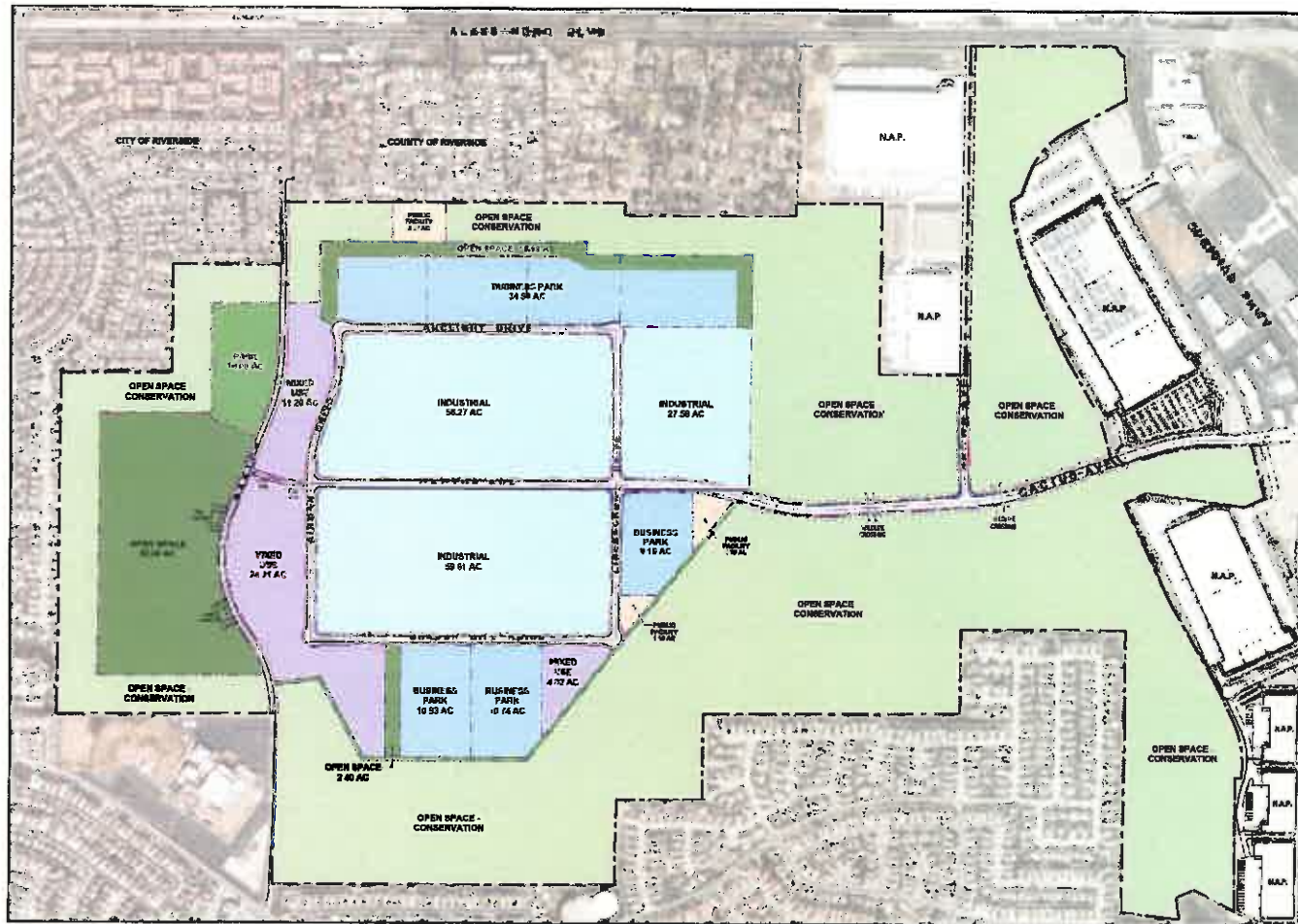
### 2.2. PURPOSE AND APPLICABILITY

The following items describe the relationship of the Specific Plan land use regulations in the context of other land use documents developed by the JPA.

1. Terms used in these regulations and guidelines shall have the same definitions as given in the MJPA Development Code ("Development Code") and the General Plan of the MJPA ("General Plan") unless otherwise defined in the Specific Plan.
2. Any details or issues not specifically covered in the Specific Plan regulations shall be subject to the regulations of the Development Code
3. The Specific Plan Land Use regulations are adopted pursuant to Section 65450 of the State of California Government Code et seq. It is specifically intended by such adaptation that the development standards herein shall regulate all development within the Specific Plan Area.

### 2.3. LAND USE COMPATIBILITY

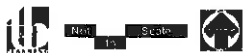
The Specific Plan establishes development patterns to limit the potential for land use conflicts, both within the West Campus Upper Plateau and in relation to other uses in the vicinity. A key consideration guiding the development is the proximity of Air Reserve Base Runway 14/32. The Riverside County Airport Land Use Commission published an Airport Land Use Plan in 1984. This plan established land use restrictions within Airport Influenced Area, which consist of imaginary surfaces extending outward from an airport's runway. In 1998, an Air Installation Compatible Use Zone (AICUZ) Study was completed to identify land use restrictions and height limitations within the Airport Influenced Area. Additional information regarding aviation regulations is provided in Section 2.5 below.

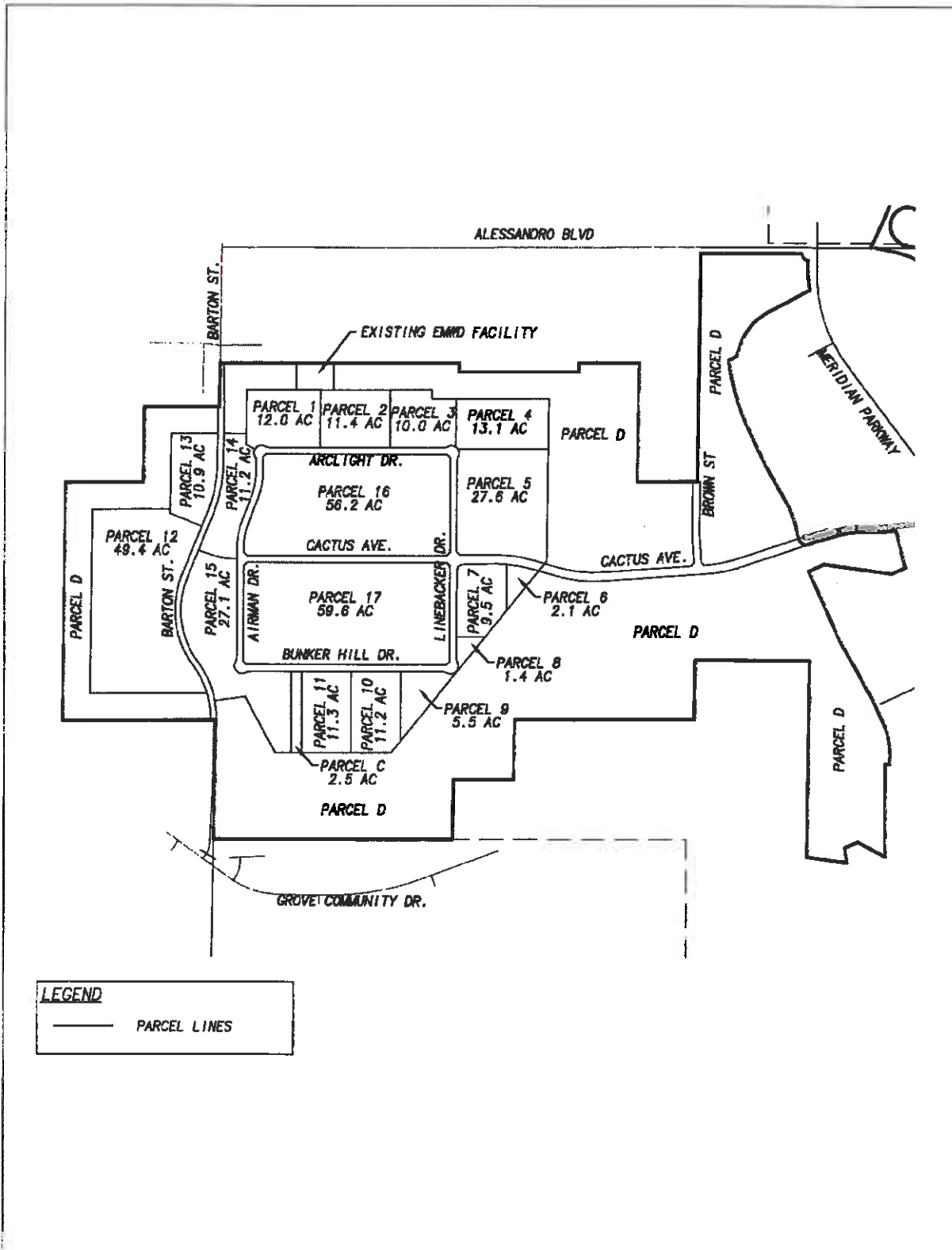


LAND USE LEGEND	
MIXED USE:	42.22 AC
BUSINESS PARK:	65.35 AC
INDUSTRIAL:	143.48 AC
STREETS:	37.7 AC
PUBLIC FACILITIES:	2.84 AC
PARK:	30.63 AC
OPEN SPACE:	67.11 AC
NET DEVELOPABLE:	389.56 AC
PUBLIC FACILITY - EXISTING:	2.84 AC
OPEN SPACE - CONSERVATION:	445.05 AC
GROSS ACREAGE:	817.80 AC

Source(s): Meridian/RGA Office of Architecture Design (02-04-2022)

Figure 2-1





Source(s): DRC Engineering (01-18-2022)

Figure 2-2



Not to Scale



Proposed TPM Exhibit

## 2.4. LAND USE PLAN

The West Campus Upper Plateau Specific Plan is an 817.9-acre master-planned industrial park that provides industrial, commercial, and office use and a substantial amount of open space and recreational use. The open space and parks are also provided for the employees of the West Campus Upper Plateau, visitors, and the surrounding residents. *Figure 2-1, Conceptual Land Use Plan*, and *Figure 2-2, Proposed TPM Exhibit*, depict the physical arrangement and the major roads within the Specific Plan Area. *Table 2-1, Land Use Plan Statistical Summary*, provides the acreages and development intensity for each land use designation within the West Campus Upper Plateau Specific Plan.

This section of the Specific Plan identifies the following five land use districts: Business Park, Industrial, Mixed Used, Park/Recreation/Open Space, and Public Facility. These districts are summarized below:

### **Business Park:**

Business Park uses include administrative, financial, governmental, and community support services; research and development centers; light manufacturing; parcel delivery terminal; vocational education and training facilities; business and trades schools; and emergency services. Business Park areas are generally served by arterial roadways, providing automobile and transit access. These areas are characterized as major employment concentrations. Development in this category, except for warehousing, is generally within a campus-like setting or cluster development pattern. Outdoor storage as a primary use is prohibited.

### **Industrial:**

Industrial may support a wide range of manufacturing and non-manufacturing uses from warehouse and distribution facilities to industrial activities. Uses supported include warehousing/distribution and assemblage of non-hazardous products and materials or retailing related to manufacturing activity; and parcel delivery terminal on no less than 60 acres. Uses may include open storage, office/industrial park; light industry; manufacturing; research and development centers; maintenance shops; and emergency services center. The area devoted to outdoor storage may not exceed the building area.

### **Mixed-Use:**

Mixed uses include a variety of complementary land uses, including commercial, business park, office, medical, educational and vocational, research and development, and services. Industrial, warehousing, and outdoor storage is prohibited.

### **Park/Recreation/Open Space:**

Park/Recreation/Open Space uses include all passive and active park or recreation areas whether private or public in the Planning Area. Active recreation activities include outdoor athletic fields and public parklands. Passive activities include natural preserves with trails, along with designated arid natural open space areas. The Park/Recreation/Open Space uses will also include civic uses such as police and fire substations.

### **Public Facility:**

Though only a limited amount of acreage is provided for this district, Public Facility uses include a wide range of public, quasi-public, and private uses such as public cultural and historical facilities, government administrative offices and facilities, public utilities, and major transportation corridors. However, land uses determined to be sensitive to, or incompatible with aviation operations shall be excluded.

Table 2-1 Land Use Plan Statistical Summary

Land Use Designation	Acres	Maximum Floor Area Ratio	Maximum Building Square Footage
Mixed Use	42.22	0.60	1,103,462 SF
Business Park	65.35	0.75	2,134,985 SF
Industrial	143.56	0.60	3,752,084 SF
Public Facilities	5.71	---	---
<b>Open Space</b>			
Park	10.88	---	---
Open Space	67.11	---	---
Open Space - Conservation	445.47	---	---
Roadways	37.70	---	---
<b>Total</b>	<b>817.90</b>		<b>6,990,531 SF</b>

## 2.5. OVERLAY DISTRICTS

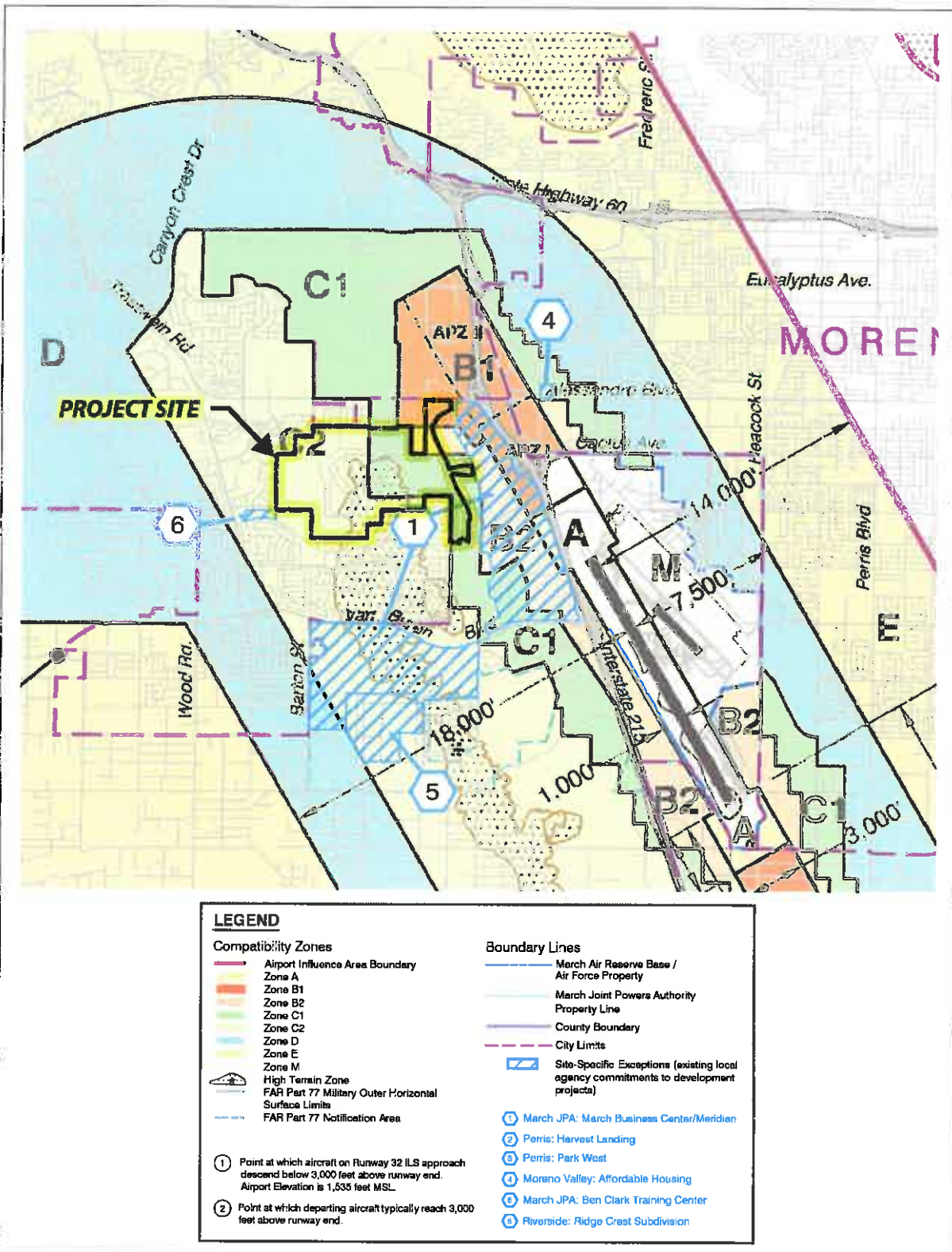
The Specific Plan area contains overlay zones within its boundaries. *Figure 2-3, MARB Land Use Compatibility Map*, depicts the location of the Inner Approach/Departure Zone (B1), High Noise Zone (B2), Primary Approach/Departure Zone (C1), and the Flight Corridor Zone (C2). To ensure consistency with the March Air Reserve Base / Inland Port Airport Land Use Compatibility Plan, this Specific Plan provides land use regulations relating to safety (both for air navigation and for people within the West Campus Upper Plateau), noise impacts, and building heights. The following paragraphs summarize these regulations.

### 2.5.1. Aviation Safety Regulations

*Figure 2-3, MARB Land Use Compatibility Map*, shows the location of the project in relationship to the various land use compatibility zones in association with the March Air Reserve Base. The project site is located within compatibility zones B1, B2, C1 and C2, with most of the project being within zones C1 and C2. Depending upon the compatibility zone, certain land uses are prohibited or discouraged due to their proximity to the airport. Any discouraged uses must be reviewed by the Riverside County Airport Land Use Commission. Additional information regarding prohibited and discouraged land uses can be found in Table MA-2 of the March Air Reserve Base / Inland Port Airport Land Use Compatibility Plan.

Regulations relating to the safety of air navigation are as follows:

- The Final Map shall convey an aviation easement to the JPA
- Lighting Plans for any development shall be reviewed and approved by the ALUC and the Air Force Reserve
- Uses that would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft during initial climb or final approach shall be prohibited
- Use of rooftop solar panels shall be reviewed and approved by the JPA
- Uses that generate smoke or water vapor which would affect safe air navigation shall be prohibited
- Uses that generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation shall be prohibited



Source(s): March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan (11-13-2014)

Figure 2-3



MARB Land Use Compatibility Map

### 2.5.2. AVIATION NOISE REGULATIONS

An acoustical analysis shall be required for any noise sensitive uses before the implementation of the following uses:

- Churches and Places of Religious Assembly
- Hotel/Motel
- Museums
- Private Clubs, Lodges, and Fraternal Organizations
- Radio and television studios
- Trade Schools

This analysis will include components necessary to achieve an indoor noise reduction level of 25 to 30 decibels for each of the Project's components with noise sensitive uses, and will include all surrounding noise sources (e.g., transportation and industrial) at their ultimate design and capacity.

### 2.5.3. AVIATION BUILDING HEIGHTS REGULATIONS

A further limitation on site development is the height of structures in the vicinity of the runway. Federal Aviation Regulations (FAR) Part 77 defines a variety of imaginary surfaces around airports, including a horizontal surface and a conical surface. FAR Part 77 is not an absolute height limit. Instead, it is a guideline used by the FAA to identify structures that may constitute a hazard to air navigation. Any construction or alteration of greater height than an imaginary surface extending upward and outward at a 100 to 1 slope from the nearest point of the runway (see FAR §77.13.2.i) will require the preparation of FAA Notice of Proposed Construction or Alteration (form 7460-1). If a hazard to air navigation is identified, then the FAA will issue a determination of hazard to air navigation. However, the FAA does not have the authority to prevent encroachment; it is up to the local land use authority to enforce the recommendation.

*Figure A-1, MARB Airspace Protection Surfaces* in Appendix A is a land use compatibility map that shows FAR Part 77 surfaces adjacent to MARB. As shown in this exhibit, terrain elevations (without structures) penetrate the conical surface on the Specific Plan Area, east of Barton Street. Depending on the elevation of the finished grade and height of the proposed structure, future development in West Campus Upper Plateau Specific Plan may penetrate the Part 77 surfaces. Development proposals in West Campus Upper Plateau will file form 7460-1 as appropriate based on §77.13.2.i.

*Figure 2-3, MARB Land Use Compatibility Map*, shows the location of the Height Caution Zone. This zone is defined as the area where the maximum allowable building height plus the rough grading plan elevation penetrates the Part 77 surface. Within the Height Caution Zone, objects up to 50 feet tall are acceptable, and do not require ALUC review for the purposes of height factors. ALUC review will be required for any proposed object taller than 50 feet within the Height Caution Zone.

## 2.6. OPEN SPACE

The West Campus Upper Plateau Specific Plan will provide approximately 445 acres of Open Space – Conservation land use, representing more than ½ of the entire Specific Plan Area. The open space areas are represented in three different land use types: park, open space, and open space-conservation. As a result, the open space will provide both active and passive use opportunities, as well as habitat value and aesthetic benefit to the area.

### **2.6.1. PARK SITE**

An approximately 10-acre park will be established in the northwestern corner of the West Campus Upper Plateau Area, west of Barton Street. This 10-acre park is intended for both active and passive use, and is conceptually designed to include two soccer fields, two basketball/pickleball courts, two exercise nodes, playground area, shaded picnic areas, restrooms, a loop trail, and parking for approximately 100 vehicles. Park use is anticipated to be from local residents and employees of businesses within the Specific Plan Area. *Figure 2-4, Conceptual Park Design*, provides a design concept for the park site.

### **2.6.2. OPEN SPACE AREA**

An Open Space area is approximately 50 acres in size will be located in the western segment of the Specific Plan Area, west of and adjacent to Barton Street. This area will be designated for hiking trails and other passive uses. The area will generally remain in its natural state, with exception to the planned hiking trails. Two trailhead locations will be provided adjacent to and west of Barton Street. Each trailhead will provide a small parking area, benches, and information kiosks for trail users. This is further represented in *Figure 2-5, Typical Trailhead Concept*.

### **2.6.3. OPEN SPACE CONSERVATION AREA**

The Specific Plan Area includes a Conservation Area as part of the Settlement Agreement that was entered between and among the Center for Biological Diversity, the San Bernardino Valley Audubon Society, MJPA, and LNR Riverside LLC in 2012. The Conservation Area will surround the West Campus Upper Plateau development area, providing a buffer to the surrounding residential area in the City and County of Riverside. A majority of the Conservation Area is proposed within the eastern segment of the Specific Plan Area. Wildlife crossing will be provided crossing under Cactus Avenue. *Figure 2-6a and 2-6b, Conceptual Wildlife Crossing Design*, shows the typical wildlife crossing designs, and *Figure 2-7a and 2-7b, Conceptual Wildlife Crossing Cross Section*, shows the typical wildlife crossing cross sections. Additionally, there are several existing recreational trails throughout the open space conservation area. The Project proposes to retain many of these trails and provide for continued public use. This is further represented in Specific Plan Section 5.2.



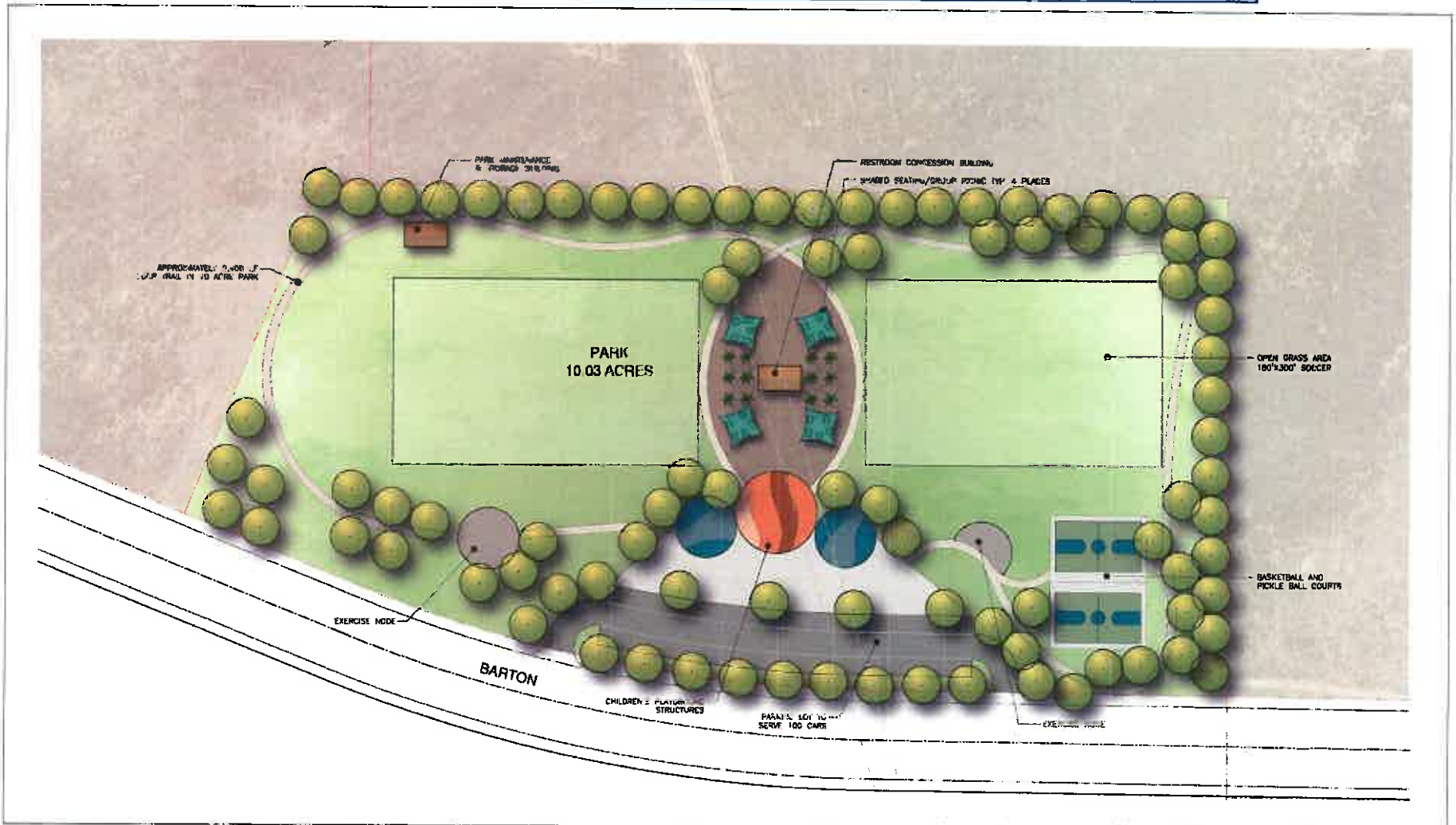


Figure 2-4



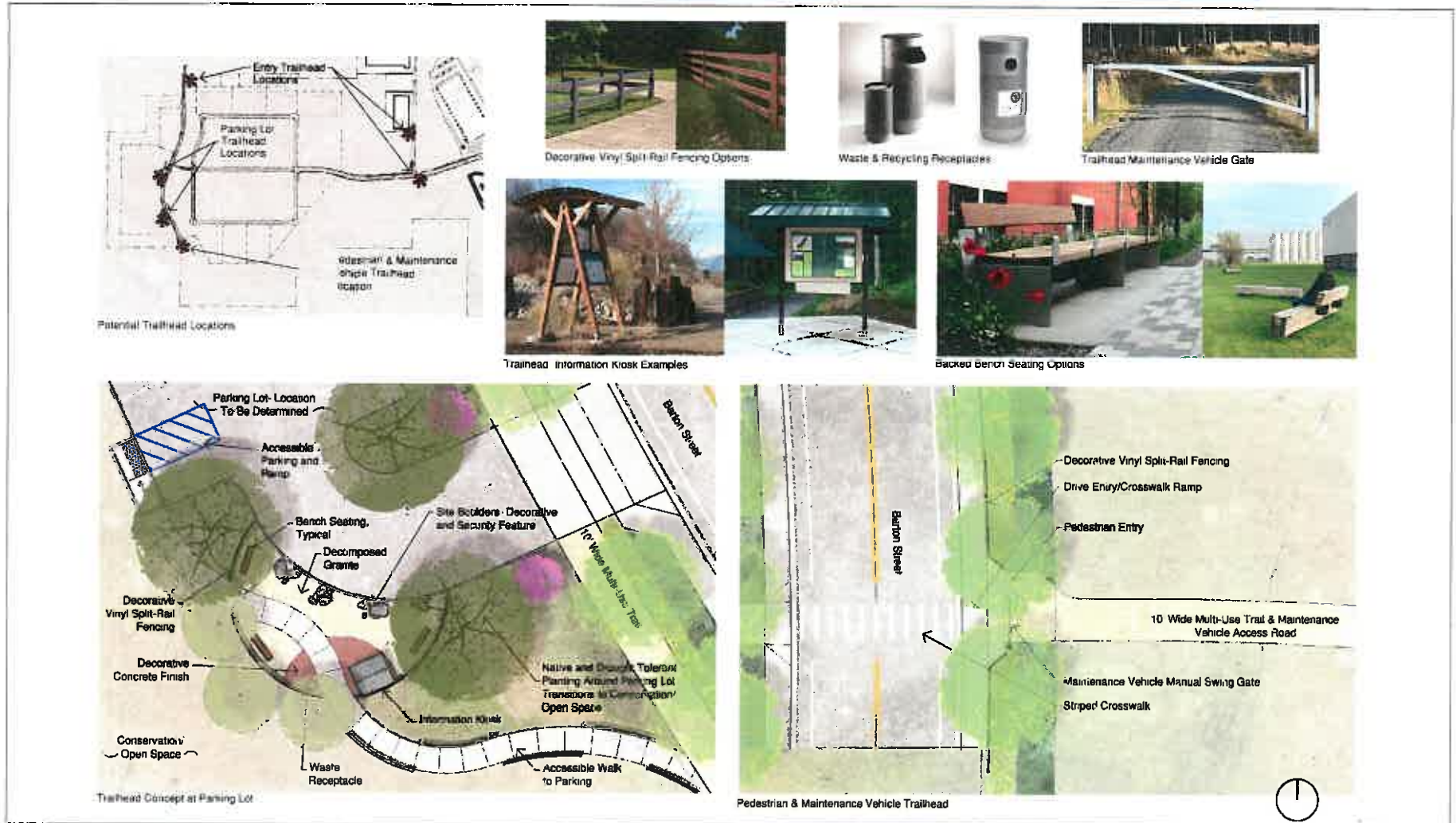
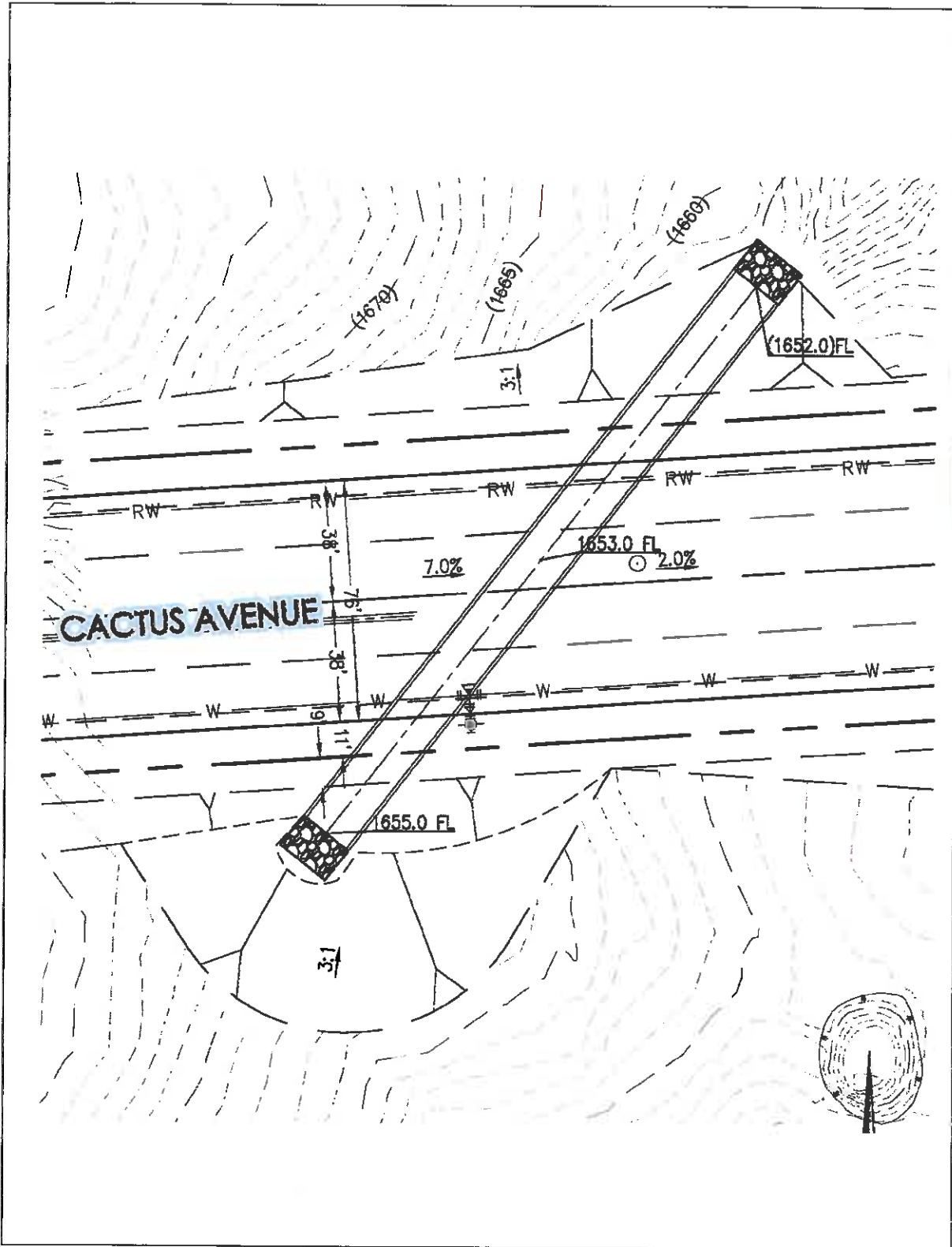


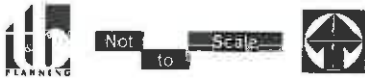
Figure 2-5



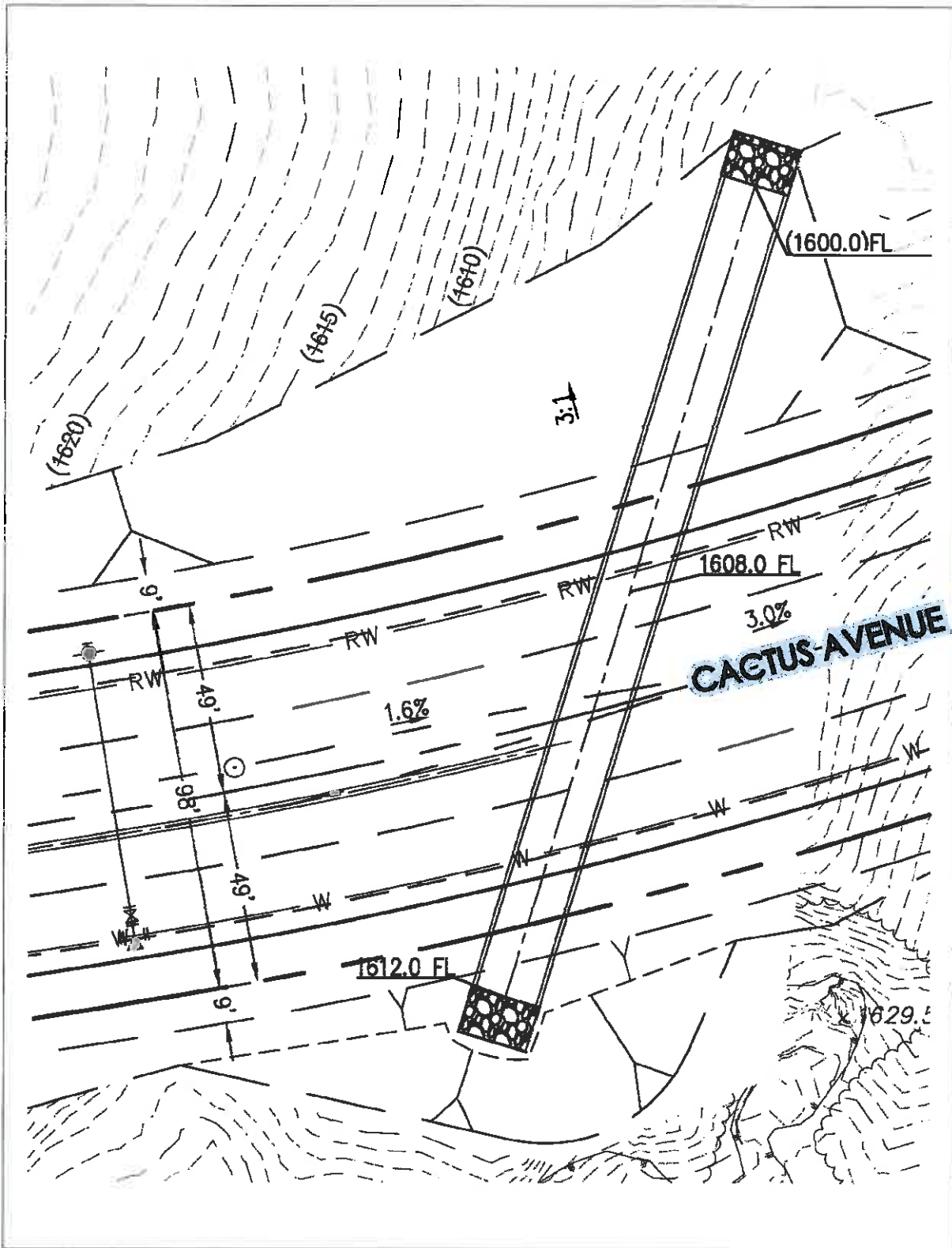


Source(s): DRC Engineering Inc (01-20-2022)

Figure 2-6A



### Conceptual Wildlife Crossing Design



Source(s): DRC Engineering Inc (01-20-2022)

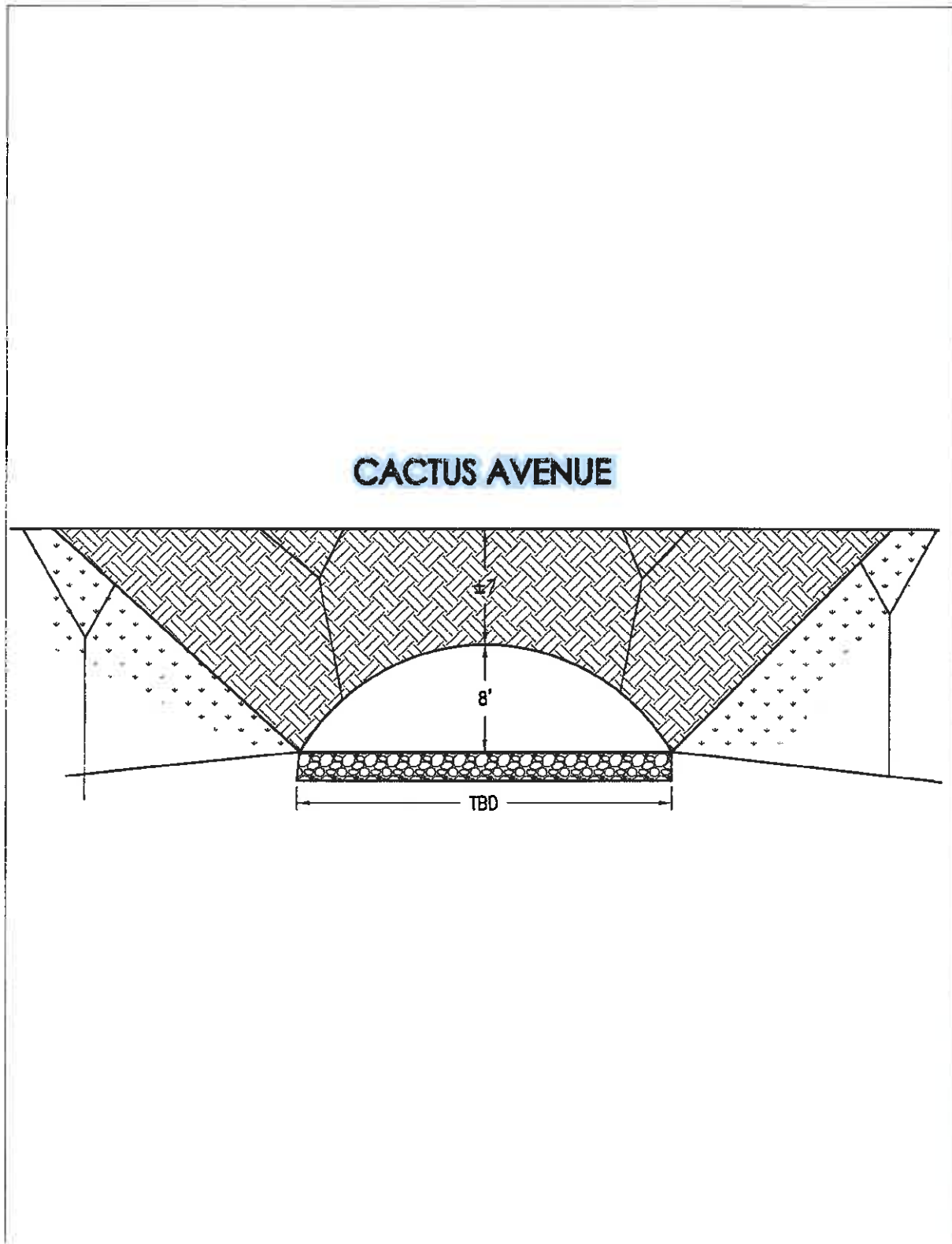
Figure 2-6B



Not to Scale



Conceptual Wildlife Crossing Design

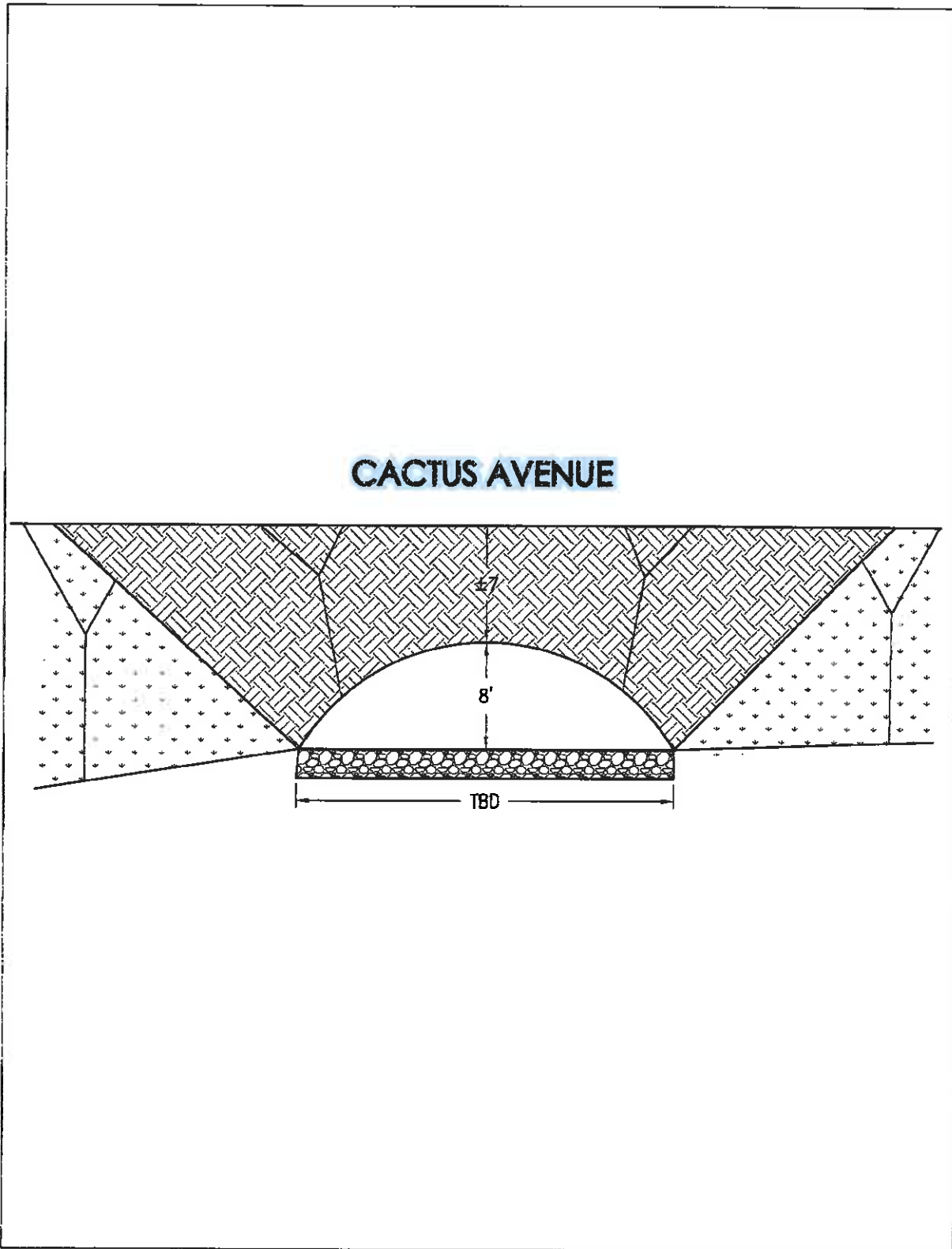


Source(s): DRC Engineering Inc (01-20-2022)

Figure 2-7A



Conceptual Wildlife Crossing Cross Section



Source(s): DRC Engineering Inc (01-20-2022)

Figure 2-7B



Not to Scale



Conceptual Wildlife Crossing Cross Section

## 3 DEVELOPMENT REGULATIONS

### 3.1. PURPOSE AND INTENT

This chapter formally establishes the various uses permitted and development standards applicable to the West Campus Upper Plateau Specific Plan. The regulations provided herein work in concert with the architectural and landscape guidelines set forth in Chapter 4 (Design Guidelines) to achieve the vision of and direction for this Specific Plan.

### 3.2. DEFINITION OF TERMS

The meanings of words, phrases, titles, and terms shall be the same as provided for in the March Joint Powers Authority Development Code, unless otherwise identified in this Specific Plan.

### 3.3. APPLICABILITY

The regulations set forth in the chapter shall apply to all development plans or agreements, tract or parcel maps, site plans, or any other action requiring administrative or discretionary approval within the West Campus Upper Plateau Specific Plan Area. Whenever the development standards contained herein differ from those contained in the March Joint Powers Authority Development Code, the provisions of this Specific Plan shall take precedence. Any development standard, condition, or situation not specifically addressed herein shall be subject to the applicable requirements of the Development Code.

### 3.4. PERMITTED, CONDITIONAL AND ANCILLARY USES

The West Campus Upper Plateau Specific Plan area and structures/facilities thereon may be developed and/or used according to those activities listed in *Table 3-1, Permitted Uses*. Table 3-1 lists the permitted, conditionally permitted, and administratively permitted land uses for each land use district established by this Specific Plan (Industrial, Business Park, Mixed-Use, and Open Space/Park and Public Facility). A use that is not listed in Table 3-1, is a prohibited use unless otherwise allowed pursuant to the procedures described in Chapter 7, *Implementation*, or applicable interpretations and determinations established by the Development Code. The entire Specific Plan Area is located within the boundary of the March Air Reserve Base Land Use Compatibility Zones (Zones B1, B2, C1, and C2), which may prohibit or restrict certain land uses. Refer to the March Air Reserve Base / Inland Port Airport Land Use Compatibility Plan for additional information.

#### LAND USE TABLE

Table 3-1 is a matrix indicating the status of specific land use types within the development districts described in Chapter 2. For each specific land use, a "P" indicates that it is permitted, and a "C" indicates that a conditional use permit is required.

**Table 3-1  
West Campus Upper Plateau Specific Plan Land Use Table**

P= Permitted; C= Conditional Use Permit; A=Ancillary; --- = Prohibited					
USES	BUSINESS PARK <sup>1,2</sup>	INDUSTRIAL <sup>3</sup>	MIXED-USE <sup>4</sup>	P/R/OS	PUBLIC FACILITY
<b>INDUSTRIAL</b>					
Bio-Medical Waste Treatment Facility	---	C	---	---	---
Manufacturing - Custom	P	P	C	---	---
Manufacturing - Light	P	P	C	---	---
Manufacturing - Medium	P	P	---	---	---
Manufacturing - Heavy	C	C	---	---	---
Newspaper Publishing Plants	P	P	---	---	---
Parcel Delivery Terminal	P	P	C	---	---
Research & Development	P	P	P	---	---
Trucking/Transportation Terminals	P	P	---	---	---
<b>WHOLESALE STORAGE/DISTRIBUTION</b>					
Public Storage/Mini-Warehouse (indoor)	C	C	C	---	---
Business Enterprise	P	P	P	---	---
Warehouse, Storage & Distribution - Medium	P	P	C	---	---
Warehouse, Storage & Distribution - Heavy	P	P	C	---	---
E-Commerce Fulfillment Center	P	P	C	---	---
<b>OFFICE</b>					
Financial Institutions	P	---	P	---	---
Government	P	P	P	---	---
Medical Clinics	P	P	P	---	---
Offices, Business & Professional	P	C	P	---	---
Regional & Corporate Headquarters	P	C	P	---	---
<b>COMMERCIAL</b>					
Agricultural Equipment Repair Shops	C	P	---	---	---
Agricultural/Nursery Supplies & Services	C	C	P	---	---
Alcoholic Beverage Outlets	C	C	C	---	---
Animal Care/Pet Hotels	P	P	C	---	---
Assembly & Entertainment	C	C	C	---	---
Automotive Parts & Accessory Sales	---	---	P	---	---
Automotive Fleet Storage	C	C	C	---	---
Automotive Service Stations	---	---	---	---	---
Automotive/Truck Repair - Major	C	P	---	---	---
Automotive/Truck Repair - Minor	P	P	C	---	---
Building & Site Maintenance Services	P	P	P	---	---
Building Contractor's Storage Yard	P	P	C	---	---
Building Material & Equipment Sales	P	---	P	---	---
Business Supply/Equip Sales/Rentals	C	C	C	---	---
Business Support Services	P	P	P	---	---
Child Care Facilities	C	---	C	---	---
Churches & Places of Religious Assembly	C	---	C	---	---
Communication Facilities, Antennas & Satellite Dishes	C	C	C	---	---
Consumer goods, Furniture, Appliances, Equipment Sales	C	---	P	---	---
Convenience Sales	C	---	P	---	---
Energy Generation & Distribution Facilities	C	C	C	---	---
Exhibit Halls & Convention Facilities	---	---	C	---	---
Fairgrounds	---	---	---	---	---
Food And Beverage Sales	A	A	P	---	---
Funeral & Mortuary Services	C	---	P	---	---



P= Permitted; C= Conditional Use Permit; A=Ancillary; --- = Prohibited					
USES	BUSINESS PARK <sup>1,2</sup>	INDUSTRIAL <sup>3</sup>	MIXED-USE <sup>4</sup>	P/R/OS	PUBLIC FACILITY
General Retail Establishments	---	---	P	---	---
Golf Courses, Driving Ranges and Pitch & Putt Courses	---	---	---	---	---
Grocery Stores	---	---	---	---	---
Health Club	A	A	C	---	---
Heavy Equipment Sales and Rentals with Outside Merchandising	C	C	C	---	---
Horticulture Nurseries & Greenhouses	C	P	---	---	---
Hospitals, Intermediate Care Facilities & Nursing Facilities	---	---	---	---	---
Hotel/Motel	---	---	---	---	---
Instructional Studios	P	P	P	---	---
Interpretive Center	P	P	P	---	---
Laundry Services	P	P	C	---	---
Maintenance & Repair	P	P	P	---	---
Major Transmission, Relay or Communications Switching Stations	P	P	C	---	---
Museums	---	---	P	---	---
Bar & Grill	---	---	C	---	---
Open Air Markets for the Sale of Agriculture-related Products & Flowers	C	---	C	---	---
Outdoor Commercial	---	---	C	---	---
Outpatient Medical Clinic	---	---	P	---	---
Parking Facilities as a Primary Use	C	C	C	---	---
Personal Services	---	---	P	---	---
Petroleum Products Storage	A	A	---	---	---
Pets & Pet Supplies	---	---	C	---	---
Private Clubs, Lodges & Fraternal Organizations	---	---	C	---	---
Radio & Television Studios	P	P	P	---	---
Recreational Facilities	A	A	C	---	---
Recycling Facilities (Outdoor Storage not to Exceed Building Area)	C	P	C	---	---
Repair Services	P	P	P	---	---
Restaurants (Fast Food)	---	---	C	---	---
Restaurant (Sit Down)	A	A	P	---	---
Social Service Institutions	P	P	P	---	---
Sundries, Pharmaceutical & Convenience Sales	---	---	P	---	---
Trade Schools	C	---	C	---	---
Vehicle, Boat and Trailer Sales	C	---	C	---	---
Vehicle Storage	C	C	C	---	---
Veterinary Clinics & Animal Hospitals	C	---	P	---	---
<b>OTHER USES</b>					
Parks and Recreational Facilities (Public)	---	---	---	P	---
Public Utility Stations, Yards, Wells and Similar Facilities, Excluding Offices	---	---	---	P	P

<sup>1</sup> Within the Business Park zone, a use permit is required for uses that provide outdoor storage in excess of 10% of the primary building gross square footage.

<sup>2</sup> A Parcel Delivery Terminal is allowed within the Industrial zone on parcels that are 50 acres or larger in size.

<sup>3</sup> Within the Industrial zone, a use permit is required for uses that provide outdoor storage in excess of the primary building gross square footage.

<sup>4</sup> A maximum of 25% of the Mixed-Use district gross square footage shall be allowed for retail uses.

\* All uses subject to the density/intensity standards and additional criteria set forth in the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan. Certain uses listed in this table may be limited in density/intensity or prohibited as a result of the Compatibility Plan standards.

### 3.5. DEVELOPMENT STANDARDS

The following standards establish the development criteria that shall apply in the Business Park, Industrial, and Mixed-Use districts of this Specific Plan. The entire Specific Plan Area is within the boundary of the March Air Reserve Base Land Use Compatibility Zones, which may limit building height, land uses, and FAR based upon the land use. Refer to the March Air Reserve Base / Inland Port Airport Land Use Compatibility Plan for additional information.

No building or portion thereof shall be erected, constructed, converted, established, altered, enlarged, nor shall any legal lot or premises be used unless the legal lot or premises and building comply with the following regulations and standards:

**Table 3-2 Development Standards**

Dimensions	Business Park	Industrial	Mixed-Use
Minimum Lot Size	1 acre	5 acres	1 acre
Street Frontage (minimum)	200 ft.	600 ft.	200 ft.
Lot Width (minimum)	200 ft.	600 ft.	200 ft.
Minimum Yards			
Front Yard Setback	20 ft.	20 ft.	20 ft.
Interior Side Yard Setback <sup>1</sup>	0 ft.	0 ft.	0 ft.
Street Side Yard Setback	20ft.	20 ft.	20 ft.
Rear Yard Setback <sup>1</sup>	0 ft.	0 ft.	0 ft.
Building Height – Max.	50 ft. <sup>2</sup>	50 ft. <sup>2</sup>	40 ft. <sup>2</sup>
Screen Wall – Max.	14 ft. <sup>3</sup>	14 ft. <sup>3</sup>	14 ft. <sup>3</sup>
Floor Area Ratio - Min. <sup>4</sup>	0.45	0.50	0.35
Site Landscaping – Min.	10%	10%	20%

<sup>1</sup>Structure shall be constructed on the property line or a minimum of 3 feet from the property line.

<sup>2</sup>Increased height up to 80 feet is permitted where all building setbacks meet or exceed the proposed building height, and subject to FAA Part 77 clearance.

<sup>3</sup>Screen wall height allowed to exceed maximum when required for noise attenuation or grade differences requiring additional screen height from public right-of-way.

<sup>4</sup>Based upon building net floor area, excluding stairwells and elevator shafts, equipment rooms, lofts or mezzanines of warehouse buildings use for equipment and conveyor systems, and floors below the first or ground floor, except when used for human habitation.

#### 3.5.1. Lot Development

- 1) Two adjoining lots which have a common interior side or rear lot line may be developed with zero side yard setbacks on the common lot line, provided that the opposite side yard setback is not less than 30 feet.
- 2) Any construction or alteration of greater height than an imaginary surface extending upward and outward at a 100 to 1 slope from the nearest point of the runway (see FAR §77.13.2.i) will require the preparation of FAA Notice of Proposed Construction or Alteration (form 7460-1).
- 3) Construction of objects taller than 50 feet in the Height Caution Zone, will require review by the Airport Land Use Commission.

#### 3.5.2. Landscaping

Landscaping design for development in the West Campus Upper Plateau Specific Plan shall be consistent with the West Campus Upper Plateau Design Guidelines. A 15-foot landscaped setback, measured from the public right-of-way, will be required for all front and side yards adjacent to public streets.

**3.5.3. Driveway Widths and Locations**

Driveway width and spacing shall be in conformance with the MIPA Development Code or as approved by the MIPA Civil Engineer.

**3.5.4. Off-Street Loading Facilities**

Loading or unloading facilities shall be so sized and located so that they do not require trucks to be located in required front or street side yards during loading and unloading activities.

**3.5.5. Special Regulations**

All uses, except for storage, loading, and outdoor work, shall be conducted entirely within an enclosed building. Outdoor work, storage of merchandise, material, and equipment is permitted in interior side or rear yards, provided the area is completely enclosed by sight obscuring walls, fences, or a combination thereof.

Fences and Walls: The design and location of fences and walls shall be the same as set forth in the West Campus Upper Plateau Design Guidelines (Chapter 4 herein).

In addition to the above, the following regulations apply:

- 1) Chain link fences shall not be used within 100 feet of a public right-of-way. Where used, chain link fences shall be vinyl coated.
- 2) Coiled, spiraled, or rolled fencing such as razor wire or concertina wire shall not be permitted.
- 3) All walls or fences within 100 feet of public right-of-way or visible from residential development shall be painted to be consistent with the project building colors (higher walls may be necessary to screen trucks and outdoor storage, consistent with the approved screening plan).



**3.5.6. Off-street Parking**

It is the intent of the West Campus Upper Plateau Specific Plan to provide minimum off-street parking requirements for passenger vehicles that appropriately accommodate parking demand. Furthermore, the MJPA General Plan represents in Transportation Element Policy 2.7 that on-street parking should be de-emphasized to both increase vehicle capacity and to accommodate bicycle access. As a result, *Table 3-3, Minimum Parking Space Requirements*, identifies minimum off-street parking requirements for general industrial, manufacturing, general warehousing, and distribution operations.

**Table 3-3 Minimum Passenger Vehicle Parking Space Requirements**

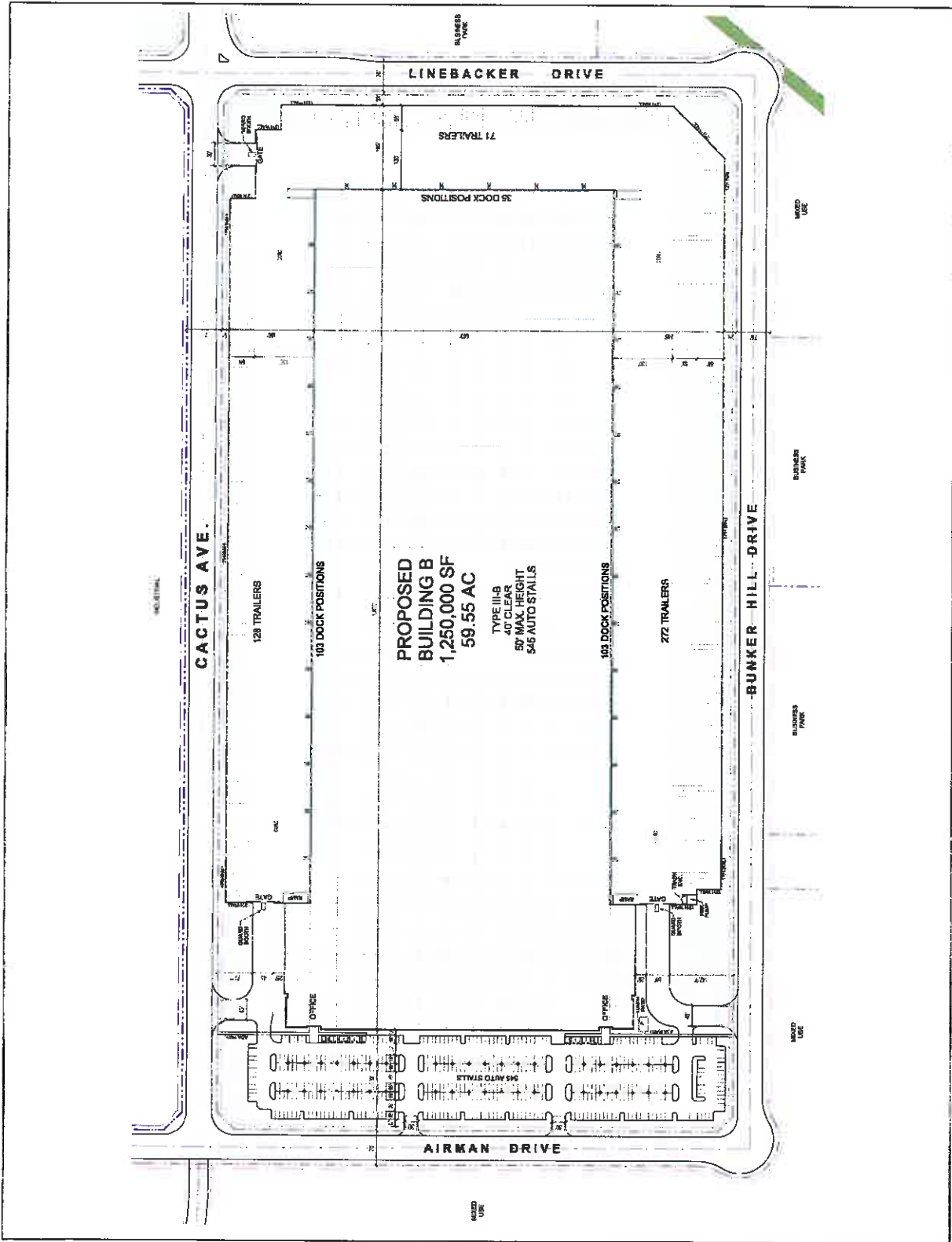
Use	Parking Spaces (per sq. ft. of Gross Floor Area) <sup>1</sup>
Light, Medium & Heavy Manufacturing First 10,000 sq. ft. 10,000-100,000 sq. ft. Over 100,000 sq. ft. Office space	1 space per 500 sq. ft. 1 space per 1,000 sq. ft. 1 space per 3,000 sq. ft. 1 space per 300 sq. ft.
Research & Development	1 space per 500 sq. ft.
Warehouse and Distribution First 20,000 sq. ft. 20,000 – 100,000 sq. ft. Over 100,000 sq. ft. Office space	1 space per 1,000 sq. ft. 1 space per 2,000 sq. ft. 1 space per 5,000 sq. ft. 1 space per 300 sq. ft.
Other Manufacturing and Warehouse Uses First 10,000 sq. ft. Over 10,000 sq. ft. Office space	1 space per 500 sq. ft. 1 space per 2,000 sq. ft. 1 space per 300 sq. ft.

<sup>1</sup>Truck trailer parking spaces may count as passenger vehicle parking spaces if so noted on an implementing development's site plan

Uses not identified above shall adhere to the March Joint Powers Authority Development Code off-street parking requirements as represented in Section 9.11.040 of the March Joint Powers Authority Development Code. It is acknowledged that certain land uses will have unique parking characteristics, based on building utilization, workforce composition, and other considerations. In these cases, the MJPA Commission may review a use permit application to reduce required parking through a detailed parking analysis.

**3.5.7. Conceptual Building Layouts**

*Figure 3-1, Conceptual Building Layout (Bldg B)*, and *Figure 3-2, Conceptual Building Layout (Bldg C)*, depict a preliminary layout of two sample, conceptual industrial buildings for the West Campus Upper Plateau Specific Plan. The figures show a conceptual first phase of development within the Specific Plan area and are representative of the development standards for the West Campus Upper Plateau Specific Plan.



Source(s): RGA

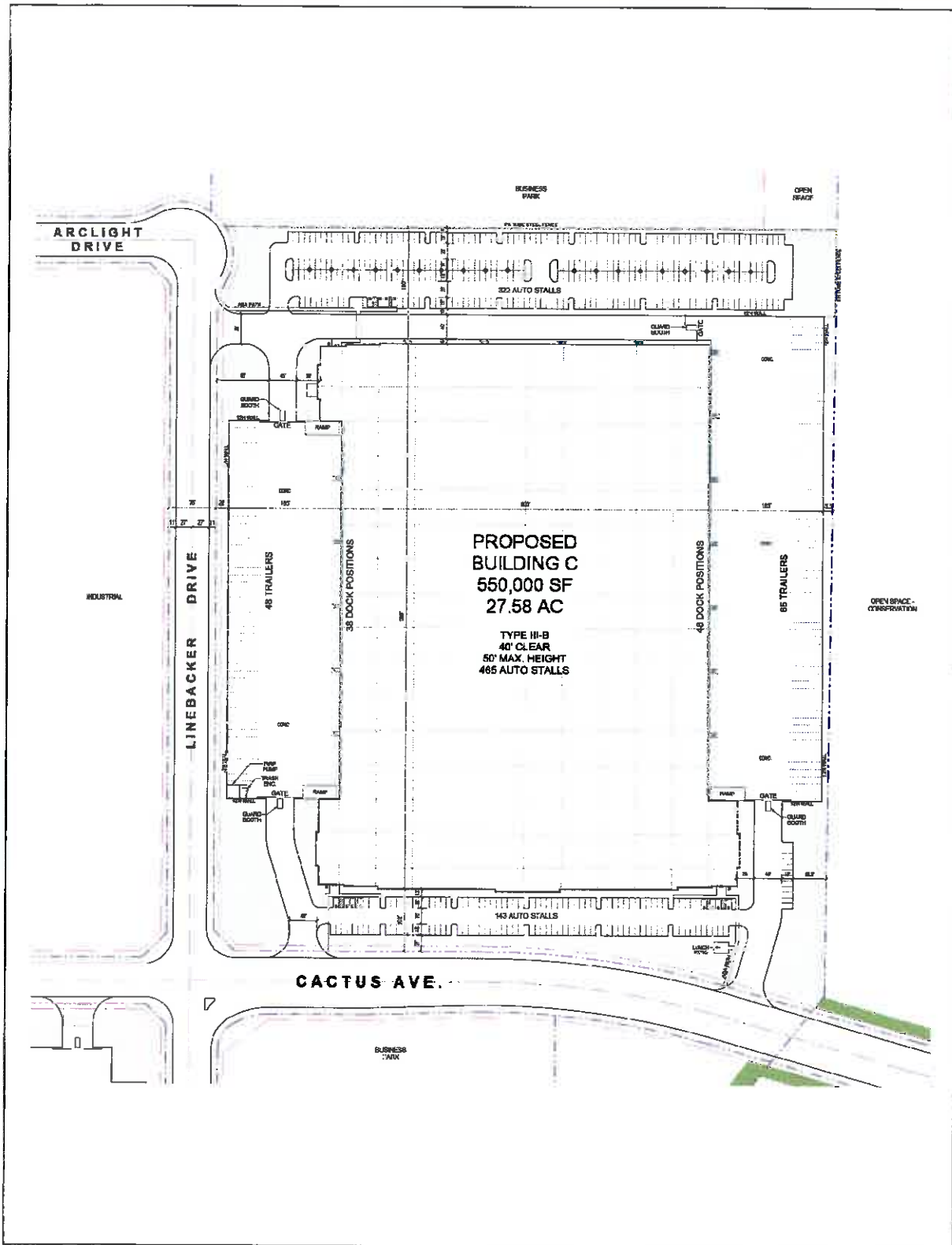
Figure 3-1



Not to Scale



Conceptual Building Layouts (Bldg B)



Source(s): RGA

Figure 3-2



Not to Scale



Conceptual Building Layouts (Bldg C)

## 4 DESIGN GUIDELINES

### 4.1. PURPOSE AND INTENT

This chapter is intended to describe the quality and character of the built environment expected for the West Campus Upper Plateau Specific Plan. While design guidelines provide aesthetic direction, they're intended to be general and provide flexibility that allows creative expression during the design of future development projects. The guidelines provide criteria for architecture, energy efficiency, lighting, signage, and landscaping.

The visual identity of the West Campus Upper Plateau Specific Plan will be primarily represented through the hardscape, landscape, and signage elements of the various developments. The architectural design guidelines contained herein are represented in a manner that ensures consistent architectural expression throughout the Specific Plan Area, while allowing a degree of flexibility for individual projects.

The Design Guidelines objectives are as follows:

- To provide the MJPAA with the assurance that the West Campus Upper Plateau will develop in accordance with the quality and character described within this Specific Plan.
- To provide guidance to developers, builders, engineers, architects, landscape architects, and other professionals to achieve and maintain the desired design quality.
- To provide an aesthetic benchmark for MJPAA staff and all other decision makers in their review of the design of future implementing development projects in the Specific Plan Area.
- To provide guidelines that convey a contemporary aesthetic theme and character while allowing flexibility for practical application and creative expression.
- To encourage energy efficiency measures that can be incorporated into the site planning, design, and construction phases of the Specific Plan's implementation.
- To ensure that the Specific Plan implements the intent of the March Joint Powers Authority General Plan and Development Code.



### 4.2. DESIGN THEME

The West Campus Upper Plateau Specific Plan is a contemporary commerce center containing Industrial, Business Park, Mixed-Use, Parks and Open-Space, and Public Facility land uses. It will provide businesses easy access to an existing regional transportation network, be in proximity to workers, as well as proximity to the Ports of LA and Long Beach (approximately 65 miles to the southwest of the project site).

The design theme for the development areas of the specific plan features an overall contemporary aesthetic, which provides architectural styling with attractive detailing, steel accents, a light-toned color palette, and timeless features. Sign designs are to be modern, landscaping colorful and drought-tolerant, project lighting focused and directed, and design features intended to lower energy use demands while encouraging efficient building operations. Development areas are surrounded by permanently preserved open space.

### 4.3. ARCHITECTURAL DESIGN GUIDELINES

The architectural style of the West Campus Upper Plateau Specific Plan emphasizes building massing over structural articulation. Buildings are characterized by simple and distinct cubic masses with interlocking wall planes, colors, and materials that work together to create visual appeal. Exterior building colors are light and gray tones with use of stone, glass, or steel materials to establish focal points, such as around building entrances and near outdoor gathering spaces. Additionally, architectural designs may mix colors, materials, and textures to articulate façades and create visual appeal.

Design elements have been selected to be compatible in character, massing, and materials that result in a clean and contemporary feel. Individual creativity and identity are encouraged, but design integrity and overall design compatibility must be maintained among all buildings and planning areas to reinforce a unified image and campus-like setting within the Specific Plan Area.



#### 4.3.1. Building Form

Building form is one of the primary elements of architecture. Numerous design aspects, including shape, mass (size), scale, proportion, and articulation, are elements of a building's "form." Building forms are especially important for building façades that are visible from Cactus Avenue at the easterly entrance into the industrial building campus area and along Barton Street.

The following guidelines apply to buildings within the West Campus Upper Plateau Specific Plan Area to ensure that development is visually consistent, appealing, and inviting. Note that building faces not visible from public roads, or publicly accessible viewing areas, are not required to adhere to the below building form guidelines.



- a. Use simple geometric shapes as the overall building form. Rectangular forms are encouraged to promote balance and visual interest. Avoid arbitrary, complicated building forms.
- b. Long horizontal wall planes visible from a public street should include periodic changes in exterior building materials, color, decorative accents, and/or articulated features.
- c. Modulation, shift and variation of building masses between adjacent buildings visible from public streets is encouraged.
- d. Main pedestrian entrances to buildings (with the exception of service doors and emergency exit doors) should be obvious through changes in massing, color, and/or building materials.
- e. Pedestrian and ground-level building entries intended for visitor use should be recessed or covered by architectural projections, roofs, or arcades in order to provide shade and visual relief.
- f. Architectural and trim detailing on building façades should be clean, simplistic, and not overly complicated.
- g. Materials applied to any elevations shall turn the corner of the building to a logical termination point in relation to architectural features or massing.

#### 4.3.2. Building Materials, Colors and Textures

Building materials and colors play a key role in creating a clean, contemporary visual environment. Therefore, the selected exterior materials, colors, and textures should complement one another throughout the West Campus Upper Plateau Specific Plan. Subtle variations are encouraged to provide visual interest.

- a. Appropriate primary exterior building materials include concrete and similar materials, as well as tilt-up panels. Primary materials should be accented by secondary materials including but not limited to natural or fabricated stone, Fire resistant wood siding (horizontal or vertical), and metal.
- b. Trim details may include metal finished in a consistent color, plaster, or concrete elements finished consistently with the building treatment. Use of overly extraneous "themed" detailing, like oversized or excessive foam cornice caps, foam molding and window detailing is discouraged.
- c. Material changes should occur at intersecting planes, preferably at the inside corners of change of wall planes, or where architectural elements intersect.
- d. Primary exterior building colors should be light and gray tones. Darker and/or more vibrant accent colors may be provided in focal point areas, such as around building entrances and near outdoor gathering spaces. Use of colors other than light and gray tones are



allowed specific to branding and being limited to not more than 10% of the exterior building surface area.

- e. Bright primary colors, garish use of color, and arbitrary patterns or stripes that will clash with this color palette are discouraged, except in signage logos.
- f. Exposed downspouts, service doors and mechanical screen colors shall be the same color as the adjacent wall.
- g. Any color banding should be vertical and not horizontal across the length of a building to deemphasize the building length and width. Short horizontal color bands are acceptable but long bands across the entire length or width of a building are discouraged.

#### 4.3.3. Windows and Doors

The patterns of window and door openings shall correspond with the overall rhythm of the building and should be consistent in form, pattern, and color within each planning area. Guidelines for windows and doors within the West Campus Upper Plateau Specific Plan are as follows:

- a. When possible, the positioning of doors and windows on individual building façades should occur in a symmetrical and repetitive pattern to create continuity.
- b. Material or color banding shall be limited in horizontal dimension in order to de-emphasize building length.
- c. Window styles and trims shall be of similar form and finished in a consistent color on each building.
- d. Unfinished/untreated metal window or door frames are prohibited. Clear silver anodized frames are allowed.
- e. Glass shall be clear or colored with subtle reflectiveness. Silver/reflective glass is prohibited.
- f. Pedestrian entry doors to buildings shall be clearly defined by features such as overhangs, awnings, and canopies or embellished with decorative framing treatments – including but not limited to accent trim. Dark and confined entries, flush doorways (except emergency exit and service doors) and tacked-on entry alcoves are discouraged.



#### 4.4. SITE FEATURES

Several key components play a critical role in the overall project design. The design of loading dock areas, placement of equipment, and screen wall and fence placement are all integral to operations and critically important to overall site aesthetics as well.

##### 4.4.1. Walls and Fences

Due to the nature of the land uses and substantial amount of open space and conservation area identified for the West Campus Upper Plateau Specific Plan, a tube steel fencing design is preferred for the fencing to be placed along the conservation/open space interface boundaries.

Additionally, four split rail fencing designs are identified for use along the Barton Street multi-purpose trail. These fencing types are represented in *Figure 4-1, Fence Details*.

Additionally, fences and walls are anticipated to be proposed in conjunction with the development of the individual project sites. Along building site perimeters and interior to building sites, fences and walls will be necessary. The final locations and details of these fences and walls will be determined when project sites and buildings are designed and oriented during implementation of the Specific Plan.

Screen wall may be provided around the perimeters of individual buildings sites and around loading and dock areas, trailer parking areas, and parking lots to screen on-site industrial uses from public views and public roads. The maximum height for these walls is expected to not exceed 14 feet (unless acoustical attenuation or grade differences from truck dock areas to public right-of-way necessitates greater height) and include landscaping in association with the wall when facing or viewed from a public street. In addition, landscaping within roadway rights-of-way and outside of rights-of-way serve as additional screening between on-site land uses and public roads.

The following guidelines for walls and fencing will ensure that these features complement the overall design theme of the West Campus Upper Plateau Specific Plan, are attractive from public viewing areas, scaled appropriately, durable, and integrated consistently within the Specific Plan Area.

- a. Freestanding walls and fences should not exceed a height of 14 feet, measured from the base of the wall/fence to the top of the wall/fence.
- b. Landscaping may be used for visual screening instead of walls and fences in locations where a solid physical barrier is not needed.
- c. Walls and fences in public view should be built with attractive, durable materials.
- d. Chain-link fencing is not permitted within 100 feet of a public right-of-way and shall be vinyl coated when used.
- e. Along public street frontages, long expanses of freestanding wall surfaces should be offset and/or architecturally treated to prevent monotony. Techniques to accomplish this may include, but are not limited to openings, material changes, pilasters and posts, and staggered sections.
- f. Wall and fencing materials shall be compatible with the design characteristics of the primary building for the site in which the wall or fence is located.

CONSERVATION / OPEN SPACE  
PERIMETER FENCING



Tube Steel Fencing

SPLIT RAIL FENCING ALONG BARTON STREET  
MULTI-PURPOSE TRAIL



Textured Matte-Finish Vinyl



Tube Steel Fencing



Textured Concrete

Figure 4-1



Not to Scale

**4.4.2. Truck Courts and Loading Docks**

- a. Loading doors, service docks, and equipment areas should be oriented or screened to reduce visibility from public roads and publicly accessible locations within the West Campus Upper Plateau Specific Plan. Screening may be accomplished with solid walls or fences that are compatible with the architectural expression of the building. Screening may also be accomplished by landscaping.
- b. No loading or unloading activity is permitted to take place from public streets/view.
- c. Adequate queuing distance should be provided on-site in front of security gates to avoid the circumstance of trucks stacking on public streets waiting to enter at gates.
- d. Truck and service vehicle entries should be designed to provide clear and convenient access to truck courts and loading areas such that passenger vehicle, pedestrian, and bicycle circulation is not adversely affected by truck movements.
- e. Loading bays that are utilized by refrigerated trailers should have dock seals and be equipped with plug-in electrical outlets.
- f. Conduit should be installed in truck courts in logical locations that would allow for the future installation of charging stations for electric trucks, in anticipation of this technology becoming available.

**4.4.3. Ground or Wall-Mounted Equipment**

- a. Ground-mounted equipment, including but not limited to mechanical or electrical equipment, emergency generators, boilers, storage tanks, risers, and electrical conduits, should be screened from public viewing areas including adjacent public roads. Screening may be accomplished with solid walls, or landscaping.
- b. Electrical equipment rooms should be located within the building envelope. Pop-outs or shed-like additions are discouraged.
- c. Wall-mounted items, such as electrical panels, should not be located on the building façade facing adjacent public roads/views. Wall-mounted items should be screened or incorporated into the architectural elements of the building so as not to be visually apparent from the street or other public areas.

**4.4.4. Rooftop Equipment**

- a. Rooftop equipment, including but not limited to mechanical equipment, electrical equipment, storage tanks, wireless telecommunication facilities, satellite dishes, vents, exhaust fans, smoke hatches, and mechanical ducts, shall be screened by rooftop screens or parapet walls so as not to be visible by the public.
- b. Integrate rooftop screens (i.e. parapet walls) into the architecture of the main building. Wood finished rooftop screens are prohibited.
- c. Building rooftops should be designed to support the future installation of solar panels. Solar rooftop systems shall be reviewed and approved by the March JPA with full consideration given to any potential glare impacts upon aviation operations.
- d. Roof access (via roof ladders or other means) must be located interior to the building.

#### 4.4.5. Trash Enclosures

- a. All outdoor refuse containers shall be screened within a permanent, lockable, and durable enclosure and should be oriented to not be visible from public roads/views. The trash enclosure design shall reflect the architectural style of adjacent buildings and use similar, high-quality materials.
- b. All outdoor trash enclosures shall be constructed with solid roofs to prevent exposure of dumpster contents to rainfall and prevent polluted stormwater runoff from these structures.
- c. Refuse collection areas shall be located behind or to the side of buildings, away from the building's main entrance and public view.
- d. Buildings shall be designed to meet all applicable state, regional and local government solid waste disposal requirements, including the requirements for Sizing of Storage, Location of Collection Area, Accessibility for Collection Vehicles, and Collection of Sorted/Diverted Waste Types.

#### 4.4.6. Outdoor Lighting

Outdoor lighting within the West Campus Upper Plateau Specific Plan is an essential architectural component that provides aesthetic appeal, enhances safe pedestrian and vehicular circulation, and adds to security. Lighting on private property within the Specific Plan should adhere to the following:

- a. Minimize glare and "spill over" light onto public streets, adjacent properties, and Conservation Area by using downward-directed lights and/or cutoff devices on outdoor lighting fixtures, including spotlights, floodlights, electrical reflectors, and other means of illumination for signs, structures, parking, loading, unloading, and similar areas. Where desired, illuminate trees and other landscape features by concealed uplight fixtures. Limit light spillover or trespass to one-quarter foot-candle or less, measured from within five feet of any adjacent property line.
- b. Lighting fixtures should have a similar design, materials, fixture color, and light color. Use of LED lighting is encouraged.
- c. Lights should be unbreakable plastic, recessed, or otherwise designed to reduce the problems associated with damage and replacement of fixtures.
- d. Neon and similar types of lighting are prohibited in all areas within the Specific Plan Area.
- e. Locate all electrical meter pedestals and light switch/control equipment in areas with minimum public visibility or screen them with appropriate plant materials.



- f. Illuminate parking lots, loading dock areas, pedestrian walkways, building entrances, and public sidewalks to the level necessary for building operation and security reasons. Dimmers and motion detectors are permitted.
- g. Along sidewalks and walkways, the use of low mounted fixtures (ground or bollard height), which reinforce the pedestrian-scaled, are encouraged.
- h. Use exterior lights to accent entrances, plazas, activity areas, and special features.
- i. High-Pressure Sodium (HPS) light fixtures are prohibited for site lighting.
- j. Lighting is prohibited that could be mistaken for airport lighting or that would create glare in the eyes of pilots of aircraft using the nearby March Air Reserve Base.

#### 4.4.7. Signage Guidelines

Signage within the Specific Plan Area serves a variety of purposes. Signs will identify the West Campus Upper Plateau Specific Plan and its building occupants and ensure the efficient circulation of vehicle traffic within the site by identifying vehicular entry points and directing vehicles to their on-site destinations. Also, signage will enhance the vehicular and pedestrian experience through the design of wayfinding components: directories, directional signage, and destination identifiers.

As such, clear, concise, and easy-to-understand signage that is also visually appealing is vitally important for a positive worker and visitor experience. General signage design standards are as follows:

- a. Signage in association with development projects should be compatible with and complementary to the building's exterior materials, colors, and finishes.
- b. The dimensions and shape of free-standing signs and sign panels or elements mounted on building façades or marquees shall be scaled proportionately to the architecture.
- c. All signs shall be contained within the parcel to which it is applicable and shall be so oriented as to preclude hazardous obstructions to person and/or vision of pedestrians and/or vehicle operators.
- d. Building occupant identification signage shall be in keeping with the character established for the Specific Plan with variations allowed to accommodate individual user identities/corporate branding standards.
- e. All signs are expected to be of the highest quality to pass eye-level examination and scrutiny.
- f. Prohibited sign components include the following:
  - i. Letters with exposed fastening and unfinished edges (unless architecturally consistent);
  - ii. Paper, cardboard, Styrofoam or untreated cloth;
  - iii. Visible moving parts or simulated moving parts by means of fluttering, rotation, or reflecting devices; and
  - iv. Flashing and strobing.

- g. All conductors, transformers, cabinets, housing, and other equipment for the illumination of signs shall be concealed and/or incorporated into the building architecture.
- h. Signs shall be constructed to not have exposed wiring, raceways, ballasts, conduit, transformers, or the like.
- i. Direction signs may be located at any vehicular or pedestrian decision point.
- j. Vehicular direction signs shall clearly direct to destination anchors within the West Campus Upper Plateau Specific Plan, on-site parking areas, and truck routes.
- k. Vehicular direction signs shall be consistent in size, shape, and design throughout the West Campus Upper Plateau Specific Plan.
- l. Typography on vehicular direction signs should be legible and have enough contrast to be read from an appropriate windshield viewing distance.
- m. Vehicular direction signs shall incorporate reflective vinyl copy for night-time illumination.
- n. All traffic control signs, whether on public or private property, shall conform to the California Manual on Uniform Traffic Control Devices (MUTCD).

#### **4.5. LANDSCAPE DESIGN GUIDELINES**

The West Campus Upper Plateau Specific Plan Landscape Design Guidelines establish landscape principles and standards that apply to all planning areas within the Specific Plan. The intent is to ensure that plant materials, monuments and entries, streetscapes, and other features are compatible with the overall design theme and that all implementing development projects are united under a common landscape design vocabulary. These Landscape Design Guidelines, when taken with the companion Architectural Design Guidelines provided herein, establish an identity for the Specific Plan that is contemporary, visually appealing, and contextually sensitive to the surrounding area.

Although a great deal of design information is presented herein, these Guidelines are not intended to establish a set of rigid landscaping requirements. It is recognized that these Guidelines will occasionally need to have flexibility to meet certain parcel-specific or user-identity requirements. As such, these Guidelines are intended to be flexible, and are subject to modification over time. However, any deviations from these Landscape Guidelines are to be in keeping with the spirit of the core elements of the overall theme described herein to ensure a cohesive and unified landscape concept.

The landscaping plan serves the dual purpose of providing visual appeal while also being sensitive to the environment and climate by using drought-tolerant materials. Landscaping occurs throughout the developed areas of the Specific Plan, being most prominent at main entry point, along roadways, and at building entrances and in passenger vehicle parking lots.

Entry Treatments welcome employees and visitors to the West Campus Upper Plateau Specific Plan. A major entry treatment will be provided on Cactus Avenue at the entrance to the developed project area. Secondary entry treatments will be provided on Barton Street near the northern and southern entry points to the Specific Plan Area.



Streetscape landscaping is proposed for all streets within the Specific Plan boundary, presenting a combination of evergreen and deciduous trees, low shrubs, and masses of groundcovers to create a visually pleasing experience for pedestrians and passing motorists.

#### **4.5.1. Plant Palette**

The plant palette for the West Campus Upper Plateau Specific Plan includes colorful shrubs and groundcovers, ornamental grasses and succulents, and evergreen and deciduous trees that are commonly used throughout Southern California and the Inland Empire region, complementing the Specific Plan's design theme and setting. Many of the plant materials are water-efficient species native to the region or naturalized to the arid Southern California climate.

A list of plant materials approved for use in the Specific Plan is provided for in *Appendix B – Landscape Plant Palette*. The plants listed establish a base palette for the landscape design. Other similar plant materials may be substituted for species listed in Appendix B, provided the alternative plants are drought-tolerant and complement the Specific Plan design theme.

To prevent or reduce wildlife hazards to aircraft operations in association with the March Air Reserve Base, plant palette priority shall be given to plants listed in the Riverside County Airport Land Use Commission's "Landscaping Near Airports" brochure, which can be found on the Commission's website ([www.rcaluc.org/Resources](http://www.rcaluc.org/Resources)). Additionally, the general planting guidelines represented in this brochure shall also be considered and incorporated into the landscape design of projects within the West Campus Upper Plateau Specific Plan.

#### **4.5.2. Irrigation**

The following general irrigation concepts shall be considered in the design and installation of irrigation systems within the West Campus Upper Plateau Specific Plan:

- a. All landscaped areas should be equipped with a permanent, automatic, underground irrigation system. Drip systems are encouraged in all areas needing irrigation.
- b. Irrigation systems should be designed to apply water slowly, allowing plants to be deep soaked and to reduce run-off.
- c. Connect the irrigation system to the recycled water conveyance system, when possible.
- d. "Pop-up" type sprinkler heads may be used adjacent to all walks, drives, curbs (car overhangs), parking areas and public right-of-way but must be designed to prevent all run-off and overspray.
- e. The design of irrigation systems, particularly the location of controller boxes, valves, and other above-ground equipment (e.g., backflow prevention devices), shall be incorporated into the overall landscaping design. Where aboveground equipment is provided, it should be screened or not placed in public view.

#### **4.5.3. Streetscapes**

Streetscape landscaping plays an important role in helping to create a sense of place. Streetscapes serve functional purposes, including screening undesirable views from public view. Within the West Campus Upper Plateau Specific Plan, streetscapes are planted with a combination of evergreen and deciduous trees, low shrubs, and masses of groundcovers to create a visually pleasing experience for pedestrians and passing motorists.

Figure 4-2, *Exhibit Key Map*, provides the location of landscape improvements, and the following provides conceptual streetscape landscape treatment details within Specific Plan Area:

a. *Cactus Avenue East Streetscape:*

Within the Specific Plan boundary, Cactus Avenue East has two street designs, though both utilize the same landscape plant palette as shown in *Figure 4-3, Cactus Avenue East Plant Palette*. The easterly streetscape segment runs between the eastern edge of the Specific Plan through the open space conservation area into the industrial campus. This segment consists of 4.5-foot-wide landscape parkways on both sides of the street. Parkway design includes a curb-adjacent park strip planted with deciduous or evergreen trees, and low flowering groundcovers and succulents, as well as a 6-foot-wide sidewalk.

The second segment of Cactus Avenue is entirely within the industrial campus, between Airman and Linebacker Drive. Parkway design includes a curb-adjacent park strip planted with deciduous and/or evergreen trees, and low flowering groundcovers and succulents, as well as a 6-foot-wide sidewalk. Evergreen and deciduous trees are planted outside of the right-of-way on both sides of the street to provide pedestrians using the sidewalk with additional opportunities for shade.

b. *Barton Street Streetscape:*

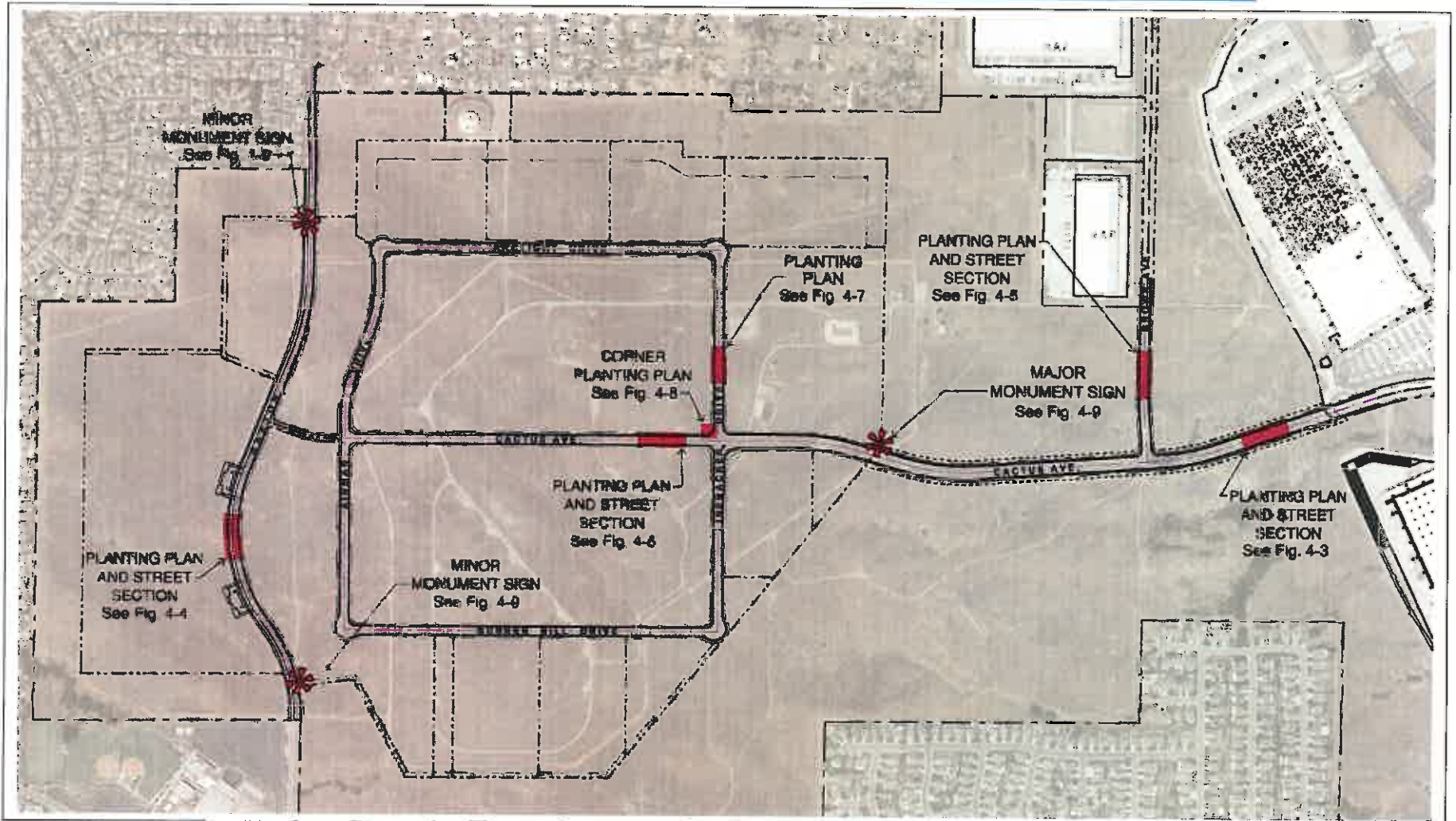
The Barton Street streetscape design includes a landscape plant palette as shown in *Figure 4-4, Barton Street Plant Palette*. Parkway design includes curb-adjacent 6-foot-wide sidewalks on both sides. A 10-foot-wide multi-purpose trail is provided along the western side of Barton Street allowing for passive recreational opportunities and connecting neighboring residential areas to the park site and open space area. A 5-foot-wide landscape area is designed between the sidewalk and multi-purposed trail, which will be planted with deciduous and/or evergreen trees, and low flowering groundcovers and succulents. A similar landscape treatment is designed along the east side of the street between the sidewalk and edge of right-of-way.

c. *Brown Avenue Streetscape:*

The Brown Avenue streetscape design includes a landscape plan palette represented in *Figure 4-5, Brown Avenue Plant Palette*. Parkway design includes a curb-adjacent 6-foot-wide sidewalks on both sides of the street. The remaining area between the sidewalk and edge of right-of-way will be planted with deciduous or evergreen trees, and low flowering groundcovers and succulents.

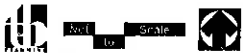
d. *Interior Streets:*

Interior streets include those within the industrial campus area, which are Arclight Drive, Cactus Avenue, Bunker Hill Drive, Airman Drive, and Linebacker Drive. Two plant palettes apply to these streets and apply depending upon north-south or east-west orientation. Thus, Arclight Drive, Cactus Avenue and Bunker Hill Drive are represented by the east-west (EW) plant palette represented in *Figure 4-6, Interior Street EW Plant Palette*, while Airman Drive and Linebacker Drive are represented by the north-south (NS) plant palette in *Figure 4-7, Interior Street NS Plant Palette*.



Source(s): Ilbisch & Associates, Inc. (01-14-2022)

Figure 4-2



Specific Plan No. XXX

Exhibit Key Map

TREES



Tuscarora Crape Myrtle  
*Lagerstroemia x 'Tuscarora'*  
Deciduous



Aleppo Pine  
*Pinus halepensis*  
Evergreen



SHRUBS

Pigeon Point Coyote Bush  
*Baccharis pilularis 'Pigeon Point'*



Little John Callistemon  
*Callistemon viminalis 'Little John'*



Magenta Rock Rose  
*Cistus x pulverulentus 'Sunset'*



Purple-leaved Hop-bush  
*Dodonaea viscosa 'Purpurea'*



Atlas fescue  
*Festuca mairei*

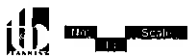


Pink Creeping Myoporum  
*Myoporum parvifolium 'Pink'*



Autumn Sage  
*Salvia greggii*

Figure 4-3



TREES



Goldenrain Tree  
*Koelreuteria paniculata*  
Deciduous

SHRUBS



Blue Grama Grass  
*Bouteloua gracilis*



Creeping Wild Rye  
*Elymus triticoides*



Atlas fescue  
*Festuca mairei*



Gray Rush  
*Juncus patens*



Bush Monkey Flower  
*Mimulus aurantiacus*



Deer Grass  
*Muhlenbergia rigens*



Black Sage  
*Salvia mellifera*



White Sage  
*Salvia apina*

Figure 4-4

TREES



London Plane Tree  
*Platanus x acerifolia*

SHRUBS



Blonde Ambition Blue Grama Grass  
*Bouteloua gracilis* 'Blonde Ambition'



Creeping Wild Rye  
*Elymus triticoides*



California Gray Rush  
*Juncus patens*



Pink Creeping Myoporum  
*Myoporum parvifolium* 'Pink'



Autumn Sage  
*Salvia greggii*

Figure 4-5

TREES



Saratoga Laurel  
*Laurus nobilis* 'Saratoga'

SHRUBS



Blonde Ambition Blue Grama Grass  
*Bouteloua gracilis* 'Blonde Ambition'



Magenta Rock Rose  
*Cistus x pulverulentus* 'Sunset'



Creeping Wild Rye  
*Elymus triticoides*



California Gray Rush  
*Juncus patens*



Dwarf Olive  
*Olea europea* 'Little Ollie'



Dwarf Red Fountain Grass  
*Pennisetum* s. 'Eaton Canyon'



Blue Chalk Stick  
*Senecio serpens*

Figure 4-6

TREES



Raywood Ash  
*Fraxinus oxycarpa* 'Raywood'  
Deciduous



Blue Grama Grass  
*Bouteloua gracilis*



Little Rev Flax Lily  
*Dianella revoluta* 'Little Rev'



Creeping Wild Rye  
*Elymus triticoides*



California Gray Rush  
*Juncus patens*



Otto Quast Spanish Lavender  
*Lavandula stoechas* 'Otto Quast'

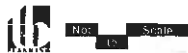


Texas Ranger  
*Leucophyllum* f. 'Green Cloud'



Little Bunny Miniature Fountain Grass  
*Pennisetum alopecuroides* 'Little Bunny'

Figure 4-7



Specific Plan No. XXX



#### 4.5.4. Entries and Monuments

The West Campus Upper Plateau Specific Plan provides for a standard street corner planting design and two-tiered hierarchy of monument signage. The entry and corner treatments are designed to provide distinctive visual statements and encourage the Specific Plan's contemporary aesthetic. All hardscape and landscape features at entry and monument locations shall provide adequate line of sight for motorists. Monumentation shall not be located within the public street right-of-way.

The typical corner landscape planting design represented in *Figure 4-8, Typical Corner Planting*, is designed to be a prominent representation of the quality and distinctiveness of the West Campus Upper Plateau Specific Plan and reinforce the general architecture and landscape theme. Entry and corner treatments should provide design flexibility to respond to physical contexts and unique circumstances of specific tenants and may differ slightly from that represented herein. However, all entry and corner treatments within the Specific Plan shall be consistent with the overall theme and character. *Figure 4-9, Monument Signage*, provides examples of typical major and minor monuments.

*a. Major Monument:*

A single Major Entry Monument is to be located on the north side of Cactus Avenue at the entrance to the industrial campus, providing entry identity for those entering the campus. The monument sign will stand approximately six feet at its highest point and be approximately 30 feet in width, including wing walls. Sign design is contemporary theme and will include finish and colors complementary to the overall design theme of the Specific Plan. Associated landscaping will be consistent with the Cactus Avenue planting plan, ensuring that plantings provide appropriate visual draw and support to the entry monument sign.

*b. Minor Monument:*

Two minor monuments are to be located on Barton Street with one place on the east side of Barton Street just north of and inside the Specific Plan boundary and the other placed on the west side of Barton Street just south of and inside the boundary. Each monument sign is to identify arrival into the Specific Plan Area. Monument sign design will be generally consistent with the major monument sign, being contemporary in appearance and of finish and colors complementary to the overall design theme of the Specific Plan. The signs will stand approximately five feet in height and 12 feet in width. Associated landscaping will be consistent with the Barton Street planting plan, ensuring that plantings provide appropriate visual draw and support to the entry monument sign.

TREES



Western Redbud  
*Cercis occidentalis*



Holly Oak  
*Quercus ilex*



SHRUBS



Blonde Ambition Blue Grama Grass  
*Bouteloua gracilis* 'Blonde Ambition'



Sageleaf Rockrose  
*Cistus salvifolius* 'prostratus'



Creeping Wild Rye  
*Elymus triticoides*

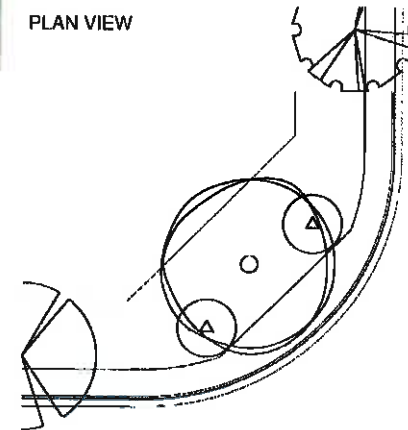


Texas Yucca - Yellow  
*Hesperaloe parviflora* 'Yellow'



California Gray Rush  
*Juncus patens*

PLAN VIEW



**PLANTING LEGEND (WUCOLS REGION 4)**

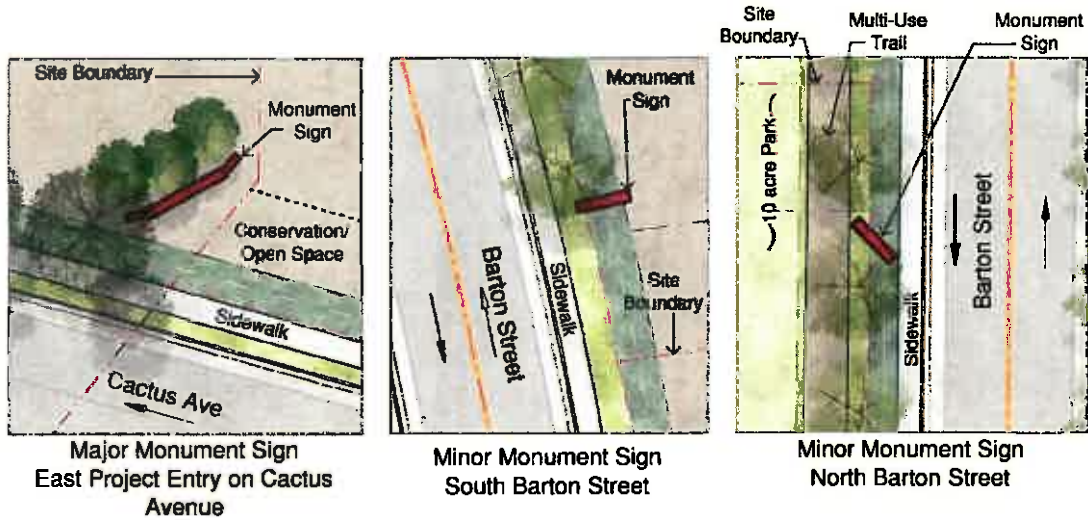
SYMBOL	ABBREVIATION	BOTANICAL NAME	COMMON NAME	SIZE	COMMENTS	WATER USE
<b>TREES</b>						
○	CERCIS	<i>Cercis occidentalis</i>	WESTERN REDBUD	36" BOX	MULTI-TRUNK	LOW/30%
○	QUERCUS	<i>Quercus ilex</i>	HOLLY OAK	36" BOX	STANDARD	LOW/30%
<b>SHRUBS</b>						
■	CIS SAL	<i>Cistus salvifolius</i> 'prostratus'	SAGELEAF ROCKROSE	5 GALLON MIN		LOW/20%
■	HES PAR	<i>Hesperaloe parviflora</i> 'Yellow'	TEXAS YUCCA- YELLOW	5 GALLON MIN		LOW/20%
<b>DRAINAGE SWALE GRASSES</b>						
■	BOU GRA	<i>Bouteloua gracilis</i>	BLUE GRAMA GRASS	5 GALLON		LOW/20%
■	ELY TRI	<i>Elymus triticoides</i>	CREEPING WILD RYE	5 GALLON		LOW/20%
■	JUN PAT	<i>Juncus patens</i>	GRAY RUSH	5 GALLON		LOW/20%

**NOTE**

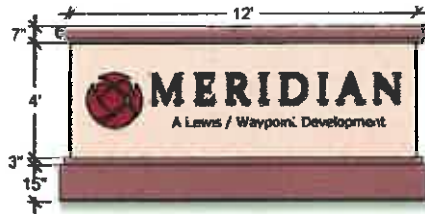
1. SALVAGE BOULDERS FOUND DURING CONSTRUCTION TO USE AS DECORATIVE ELEMENTS AT ALL CORNER PLANTERS.

Figure 4-8

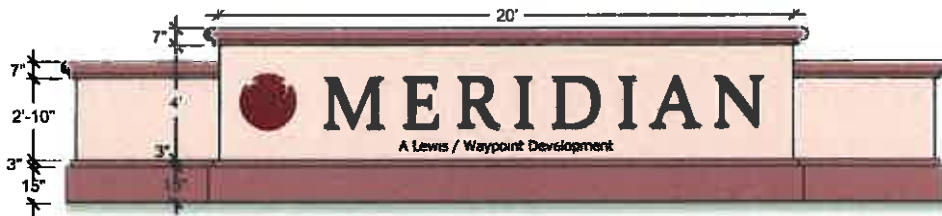




MONUMENT SIGN EXAMPLES



12' Long Minor Monument Signs at Barton Street, North and South Entries



20' Long Major Monument Sign with Wing Walls at Cactus Avenue Site Entry

Figure 4-9



Not to Scale

Monument Signage

#### **4.5.5. Open Space Areas**

Most of the land within the West Campus Upper Plateau Specific Plan is identified as open space and open space-conservation. With exception to passive use activity proposed within the open space area west of Barton Street, the open space areas are primarily intended to maintain landscaping and an overall aesthetic consistent with the current undeveloped environment.

The passive use activity to be provided for and encouraged in the open space area west of Barton Street is of benefit to the neighboring residents, employees, and visitors to the Specific Plan. A limited number of amenities are to be provided, namely consisting of two trail parking areas and meandering decomposed granite or native material walkways and trails, which is further described in Section 2.6.

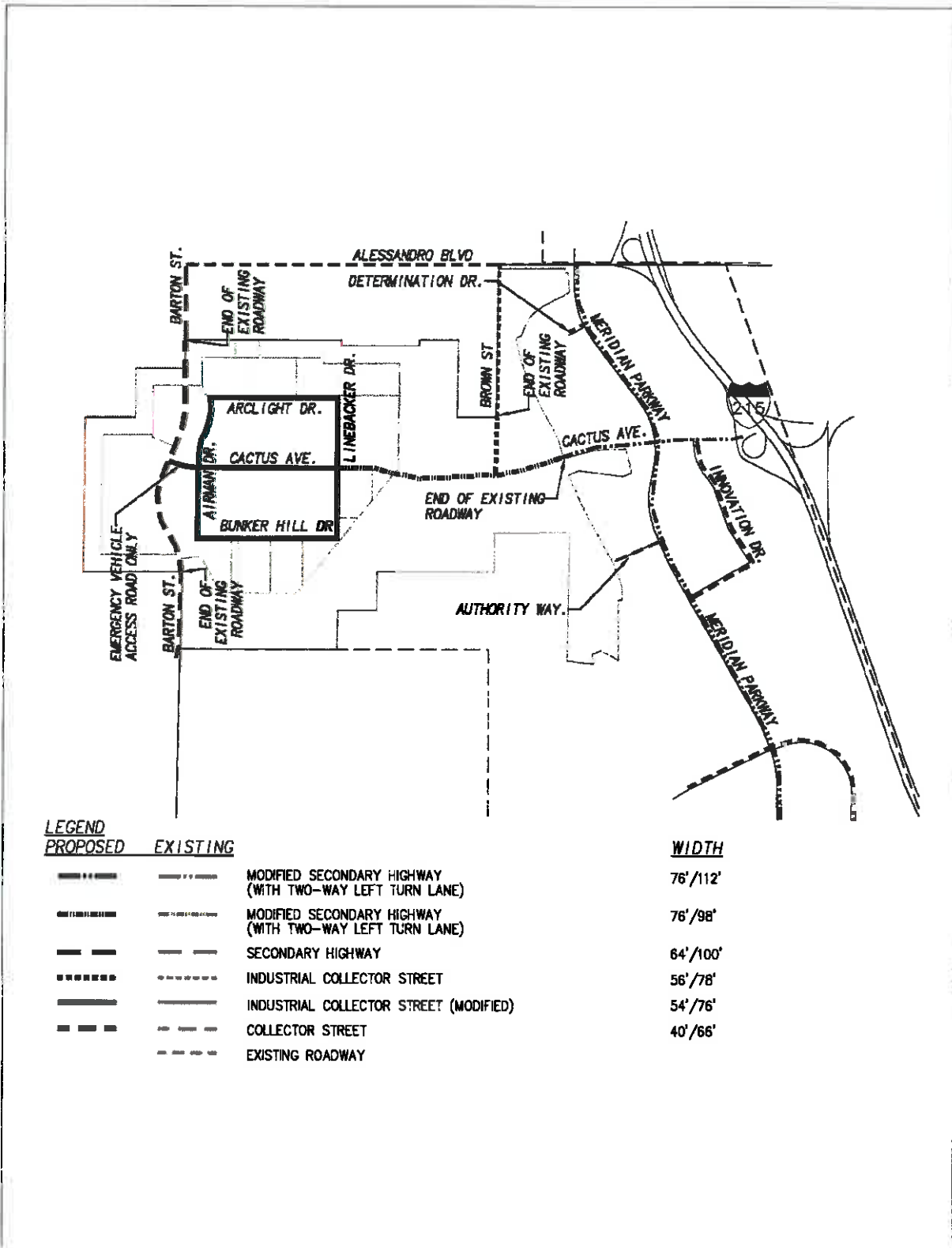
## 5 TRANSPORTATION

### 5.1 TRAFFIC CIRCULATION PLAN

The West Campus Upper Plateau Circulation Plan provides convenient, direct, and safe access for employees, visitors, and goods movement to and from the Specific Plan Area. This is achieved through a roadway network consisting of a hierarchy of local, collector and arterial streets providing access to and from the parcels comprising the West Campus Upper Plateau Specific Plan Area. The internal street network will consist of public roadways maintained by the County of Riverside. Off-site transportation improvements will be provided as deemed necessary by the MJPA to ensure there is sufficient capacity to accommodate future traffic. Improvements associated with each development phase will be assured to the satisfaction of the MJPA prior to the occupancy of that phase.

#### 5.1.1 VEHICULAR CIRCULATION

Access to and from the West Campus Upper Plateau Specific Plan Area will be provided via three roadways: Cactus Avenue, Brown Avenue and Barton Street. This is depicted in *Figure 5-1, Circulation Plan*. Cactus Avenue will serve as the primary serving roadway, connecting with I-215 located approximately 1 mile east of the Specific Plan boundary. Brown Avenue will serve as a secondary access to the industrial center, connecting with Alessandro Boulevard to the north. Barton Street will be restricted to providing access to the westerly segment of the Specific Plan Area only to the park, open space and mixed-use areas on the east and west sides of Barton Street. Barton Street will connect with the existing street network to the north and south of the Specific Plan boundary, serving as a local connector pursuant to the City of Riverside General Plan Circulation Element. In order to prevent truck trips from impacting neighboring residential streets, Barton Street will not have any connection with Cactus Avenue, strategically preventing direct vehicular access to the industrial campus area, except for emergency the inclusion of an emergency vehicle access road between Cactus Avenue and Barton Street.



Source(s):

Figure 5-1



Not to Scale



Circulation Plan

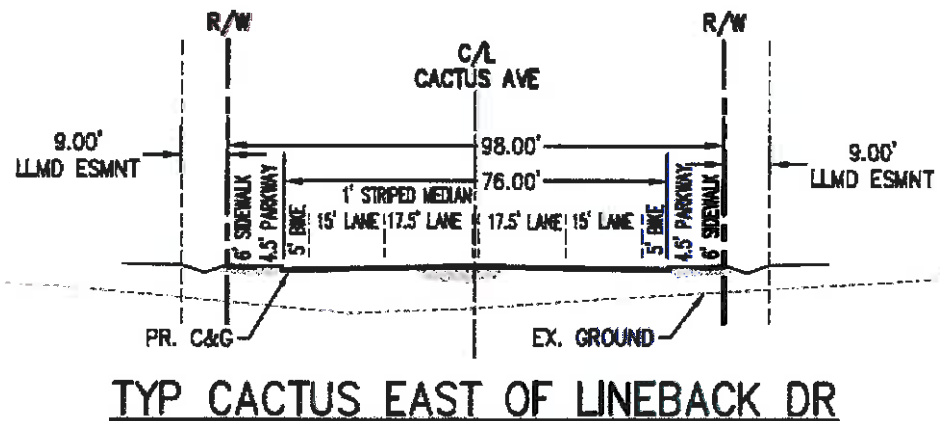
The following information further describes and illustrates the vehicular circulation network.

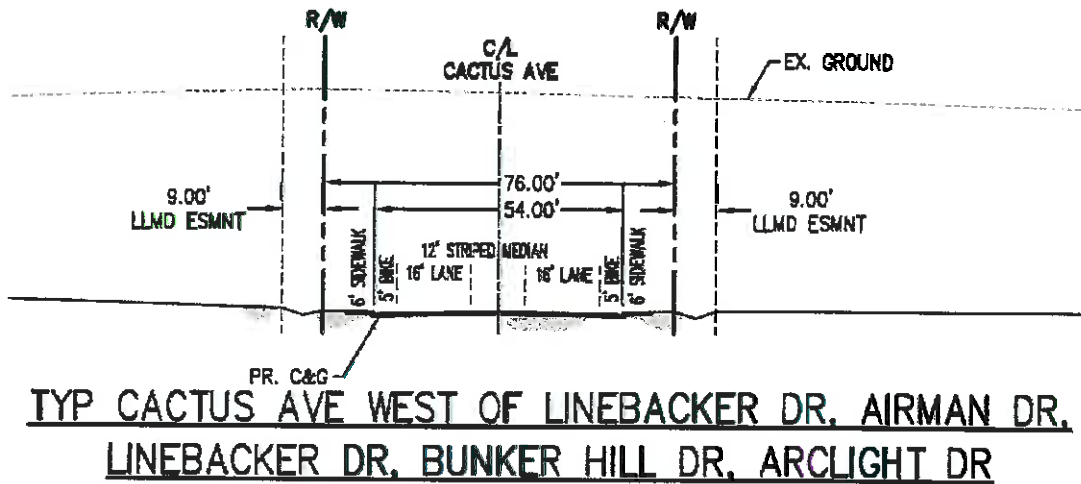
a. *Cactus Avenue:*

Cactus Avenue serves as the main access to the West Campus Upper Plateau Specific Plan. The public roadway will connect directly with the existing segment of Cactus Avenue to the east of the Specific Plan boundary, providing direct access to Meridian Parkway, I-215, and points further east. This roadway will consist of two design segments, serving as a modified secondary highway from the Specific Plan boundary westerly to Linebacker Drive, then serving as a modified industrial collector street within the industrial campus area.

The modified secondary highway segment will consist of a 98-foot wide right of way and 76 feet of curb-to-curb pavement width providing for a 17.5-foot and 15-foot travel lane in each direction, and a 5-foot bike lane in each direction. Additionally, a 6-foot sidewalk and 4.5-foot landscape parkway will also be provided within the street right of way.

The modified industrial collector segment of Cactus Avenue will consist of a 76-foot wide right of way with 54 feet of curb-to-curb pavement width providing for a single 16-foot travel lane in each direction, a 12-foot striped median and a 5-foot bicycle lane in each direction. A 6-foot curb adjacent sidewalk will be provided on each side of the street. The remaining right-of-way and an abutting 9-foot Lighting and Landscaping Maintenance District (LLMD) easement will provide for a 14-foot-wide abutting landscape parkway buffer.



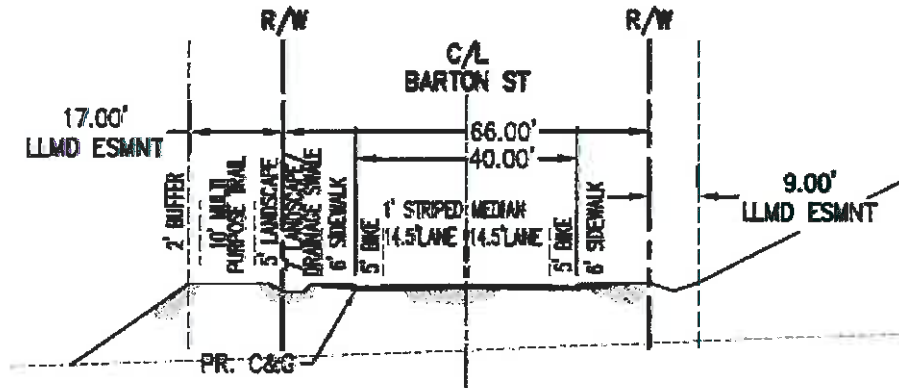


*b. Barton Street:*

Barton Street provides access to the westerly segment of the Specific Plan Area, serving the park, open space recreation area, and the western edge of the main Mixed-Use area. Barton Street will connect with the existing City of Riverside street network to the north and south of the Specific Plan boundary. This will allow for local access between the established residential neighborhoods and commercial areas in the Mission Grove community to the north and Orangecrest community to the south. Barton Street will not connect with Cactus Avenue, preventing direct vehicular access to and from the industrial campus area, except for emergency vehicles.

This roadway will be a 66-foot Collector design classification pursuant to the City of Riverside General Plan Circulation Element, consisting of a 66-foot wide right of way with 40 feet of curb-to-curb pavement width providing for a single 14.5-foot travel lane, a 1' striped median, and a 5-foot bicycle lane in each direction. A 6-foot curb adjacent sidewalk will be provided on each side of the street. In addition, a 17-foot wide LLMD easement will exist along the west side of the roadway, providing for a 10-foot-wide multi-purposed trail, as well as a 5-foot landscape buffer that will be associated with a 7-foot-wide landscape buffer and drainage swale located within the street right-of-way. The multi-purpose trail is consistent with the City of Riverside's Trails Master Plan.



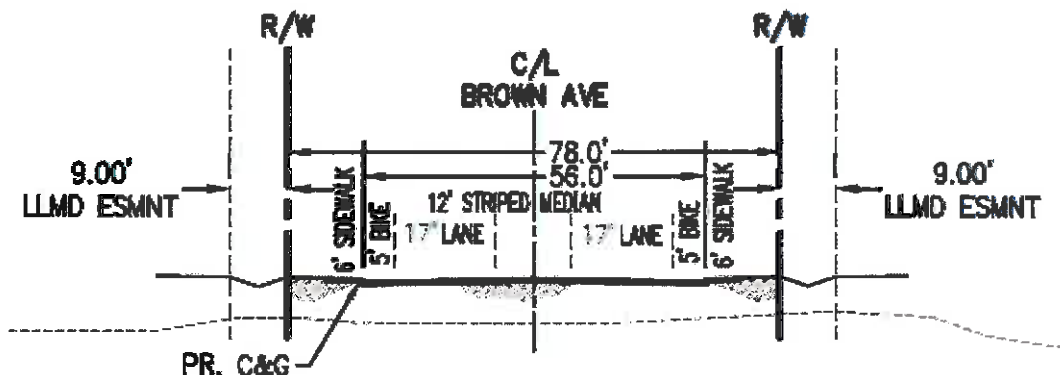


**TYP BARTON ST**  
 (WEST SIDE CONFIGURATION FOR PORTIONS ALONG OPEN SPACE/PARK ONLY)

c. Brown Avenue:

Brown Avenue serves as a secondary access to the Specific Plan Area, providing connection between Cactus Avenue and Alessandro Boulevard to the north. Though most vehicular trips serving the industrial campus area of the Specific Plan are expected to utilize Cactus Avenue and connect with Meridian Parkway and I-215, Brown Avenue will provide an alternative ingress/egress point to these connections, as well as westerly connections via Alessandro Boulevard.

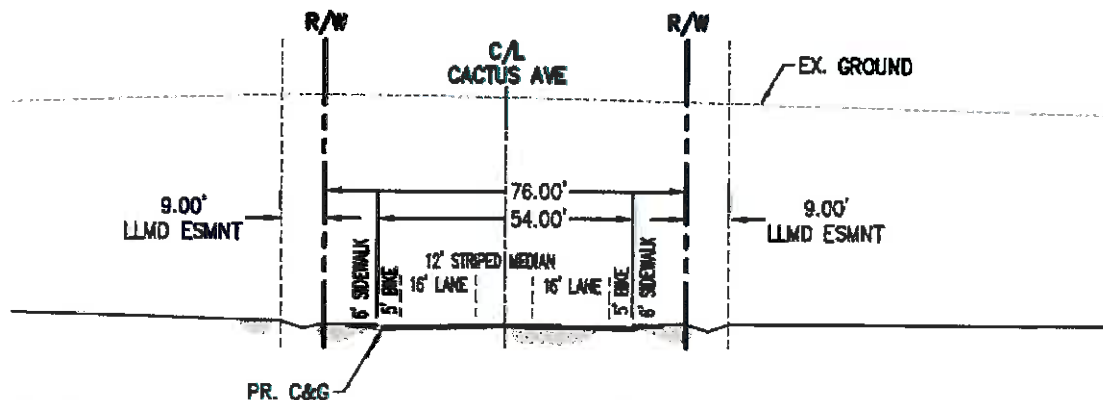
The roadway will be an industrial collector street design, consisting of a 78-foot-wide right-of-way with 56 feet of curb-to-curb pavement width providing for a single 17-foot travel lane in each direction, a 12-foot striped median and a 5-foot bicycle lane in each direction. A 6-foot curb adjacent sidewalk will be provided on each side of the street. The remaining right-of-way and an abutting 9-foot LLMD easement will provide for a 14-foot-wide abutting landscape parkway buffer.



**TYP BROWN AVE**

d. Industrial Campus Interior Streets (Arclight Drive, Airman Drive, Bunker Hill Drive, Cactus Avenue, Linebacker Drive):

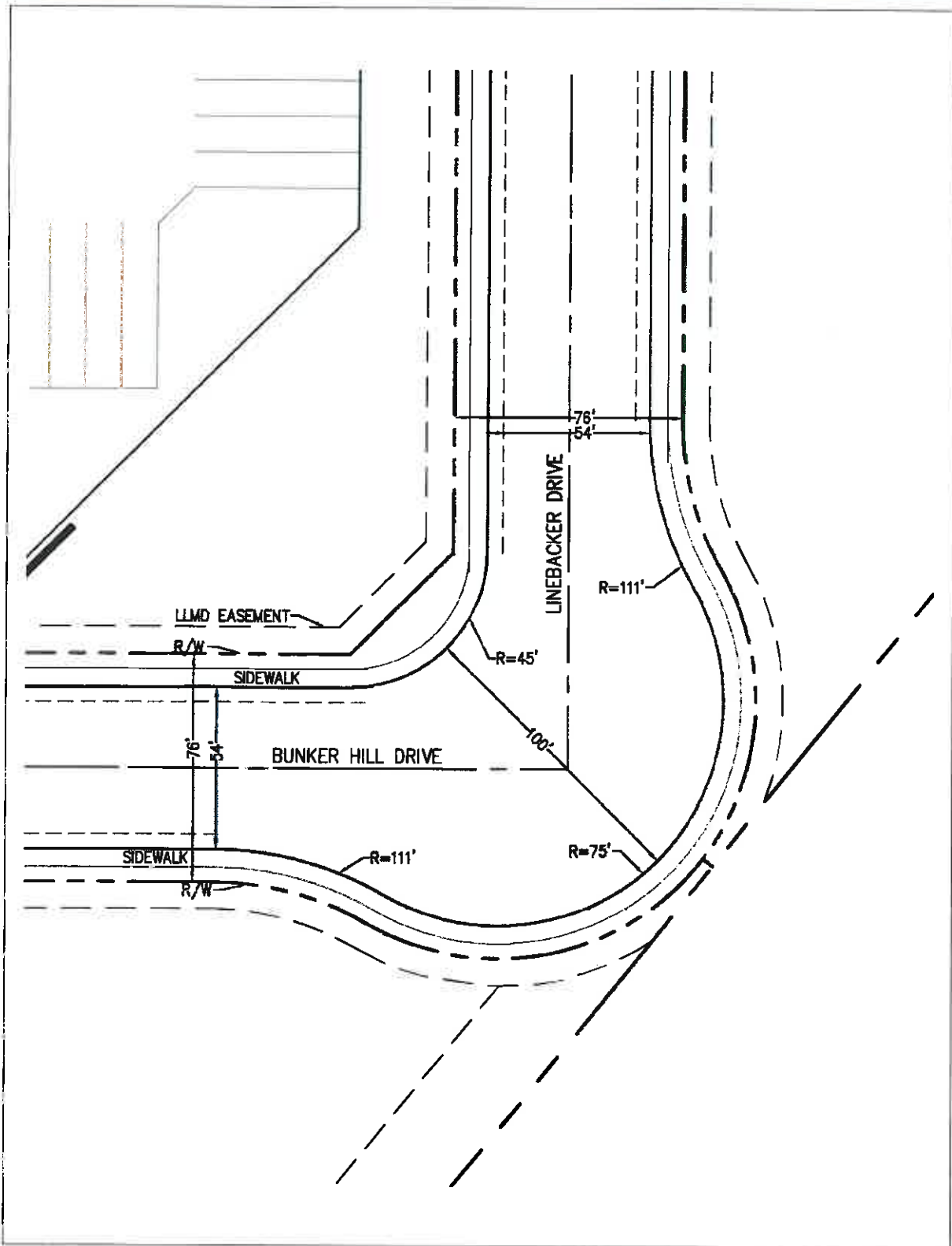
The industrial campus interior streets provide direct access between the industrial campus area and Cactus Avenue. The roadway will be a modified industrial collector street design, consisting of a 76-foot-wide right-of-way with 54 feet of curb-to-curb pavement width providing for a single 16-foot travel lane in each direction, a 12-foot striped median and a 5-foot bicycle lane in each direction. A 6-foot curb adjacent sidewalk will be provided on each side of the street. The remaining right-of-way and an abutting 9-foot LLMD easement will provide for a 14-foot-wide abutting landscape parkway buffer.



**TYP CACTUS AVE WEST OF LINEBACKER DR, AIRMAN DR,  
LINEBACKER DR, BUNKER HILL DR, ARCLIGHT DR**

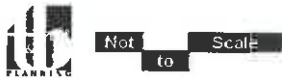
e. Industrial Campus Interior Knuckle Design:

The industrial campus interior knuckle represents the condition at the north to south and east to west roadway transition points between the campus interior streets. The interior knuckles will provide a 100-foot-wide knuckle radius to accommodate for truck turning. For example, *Figure 5-2, Knuckle Concept Designs*, provides a conceptual knuckle design between Linebacker Drive and Bunker Hill Drive.



Source(s): DRC Engineering

Figure 5-2



Knuckle Concept Designs

### 5.1.2 TRANSPORTATION DEMAND MANAGEMENT

While the West Campus Upper Plateau will provide a regional transportation benefit, much of the traffic accessing the site will be concentrated in peak commuting hours causing potential congestion. Transportation Demand Management (TDM) strategies will be implemented to shift trips outside the standard commuting hours and/or to non-“drive alone” modes of travel. This is accomplished through various employer-initiated measures, such as flexible working hours, encouragement of carpooling, and facilitating access for non-motorized (i.e., bicycling or walking) modes of travel. The following TDM measures are recommended:

1. The MJPA shall coordinate with the RCTC as the project Transportation Management Agency (TMA). The purpose of the TMA will be to:
  - Provide information on employee matching for carpools and van pools
  - Identify park and ride lot locations
  - Provide information on and encourage transit use
2. Each employer with more than 250 full-time employees shall submit a TDM plan to the JPA. The TDM plan shall address the following:
  - Designate a TDM coordinator
  - Provide a space (e.g., kiosk, bulletin board, etc.) for rideshare information
  - Provide preferential parking for carpools
  - Identify bus routes and bicycle facilities in the vicinity
  - Provide flexible working hours and/or a telecommuting program (to the extent feasible)
  - Bicycle storage facilities
  - Showers and locker room (optional)

### 5.1.3 TRUCK TRAFFIC

Industrial, business park, warehousing, and related uses typically generate a higher volume of truck traffic than other types of uses. The large size and acceleration/deceleration characteristics of trucks have a disproportionate impact on transportation capacity, as compared to passenger vehicles. In order to reduce the impacts of trucks on neighboring residential serving streets, Brown Street and Cactus Avenue will serve as the project site access for trucks. Truck traffic is discouraged from using Barton Street with no direct vehicular connection to Cactus Avenue being provided. *Figure 5-3, Truck Route*, represents streets identified for truck activity.

The project will cooperate with the City of Riverside to support measures to restrict the use of residential collector streets and secondary highways by trucks. Design of pavement sections will provide a structural depth sufficient for anticipated truck traffic. Key access intersections shall be designed to accommodate truck turns.

## 5.2 NON-AUTOMOBILE CIRCULATION

### 5.2.1 TRANSIT SERVICE

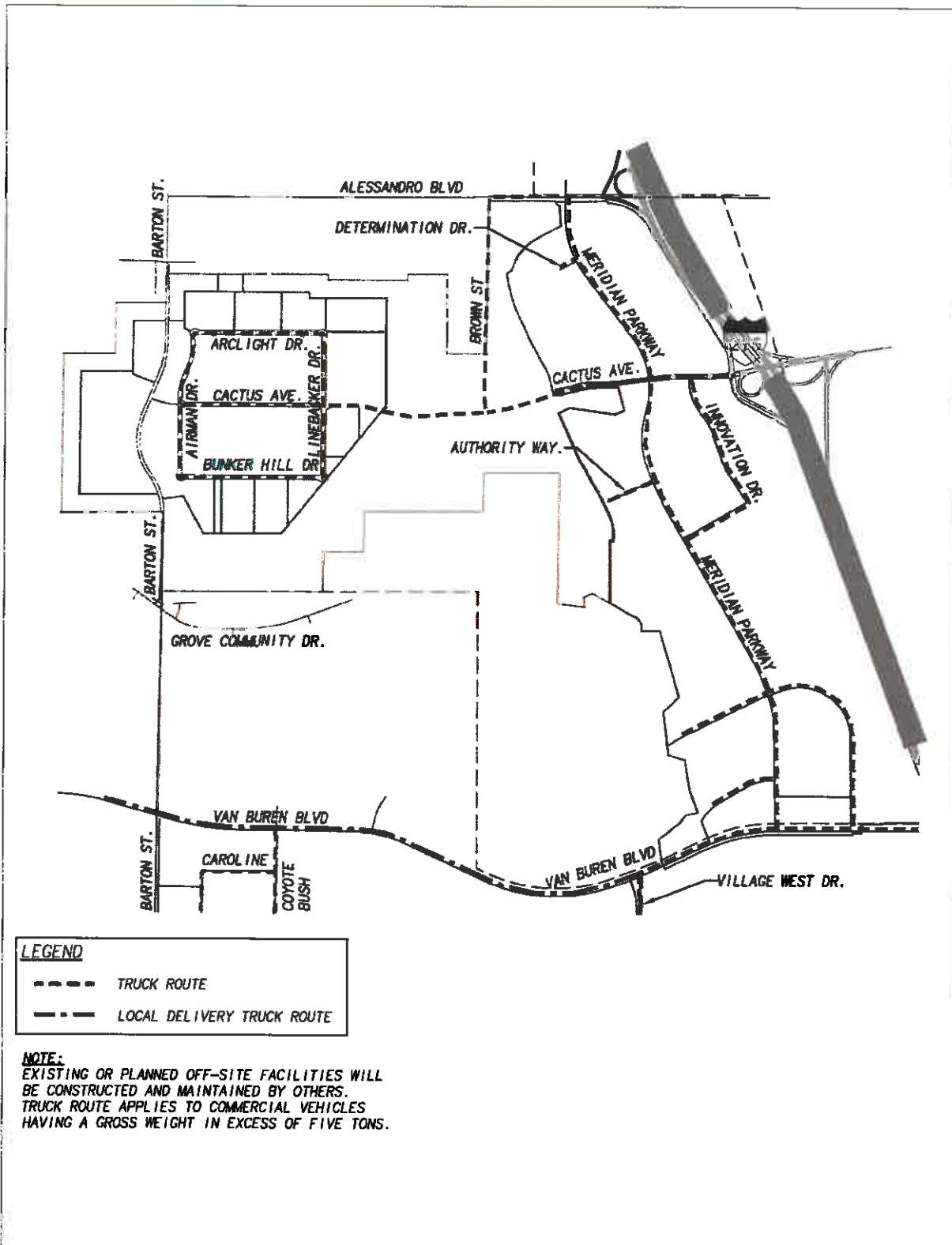
The West Campus Upper Plateau Specific Plan Area is within the Riverside Transit Agency service boundary. Bus transit service is currently provided near the Specific Plan, along Alessandro Boulevard and Orange Terrace Parkway. Additionally, a Metrolink rail stations is located on Meridian Parkway approximately 1.5 miles from the industrial campus area. As a result of the proximity to existing rail transit service and the anticipation of future bus transit service within the

Specific Plan Area, bus improvements, such as bus turnouts, bus stops, and terminals should be considered as part of the conditions of development for land uses that have a large number of employees.

### **5.2.2 BICYCLE/PEDESTRIAN ACCESS**

Bicycle and pedestrian linkages will help implement the trip reduction strategies outlined in the Transportation Demand Management section as well as provide recreational opportunities for employees and visitors to the Project. The proposed network will consist of Bike Lanes (Class II facilities), which are designated by signs and traverse the shoulder of the roadway, a 10-foot-wide multi-purpose trail, and recreational trails. Roads within the Specific Plan Area will all contain 5' wide Class II bicycle lanes and 6' wide sidewalks. A 10-foot-wide multi-use trail will be built along the western side of Barton Street. Additionally, several existing recreational trails will be retained and maintained throughout the open-space and open-space conservation areas. *Figure 5-4, Non-motorized Path of Travel Schematic*, shows the path of travel for bikes, sidewalks, and trails throughout the Specific Plan Area. The linkages were identified based on the following criteria:

1. The network was defined based on 1) connectivity to the Metrolink station east of the project area, and 2) likely routes of travel between West Campus Upper Plateau recreational amenities and existing residential development in proximity to the project area.
2. The proposed routes will provide linkages to bicycle and pedestrian facilities identified by adjacent jurisdictions.
3. Class II facilities should be provided on internal streets to facilitate access to project land uses.

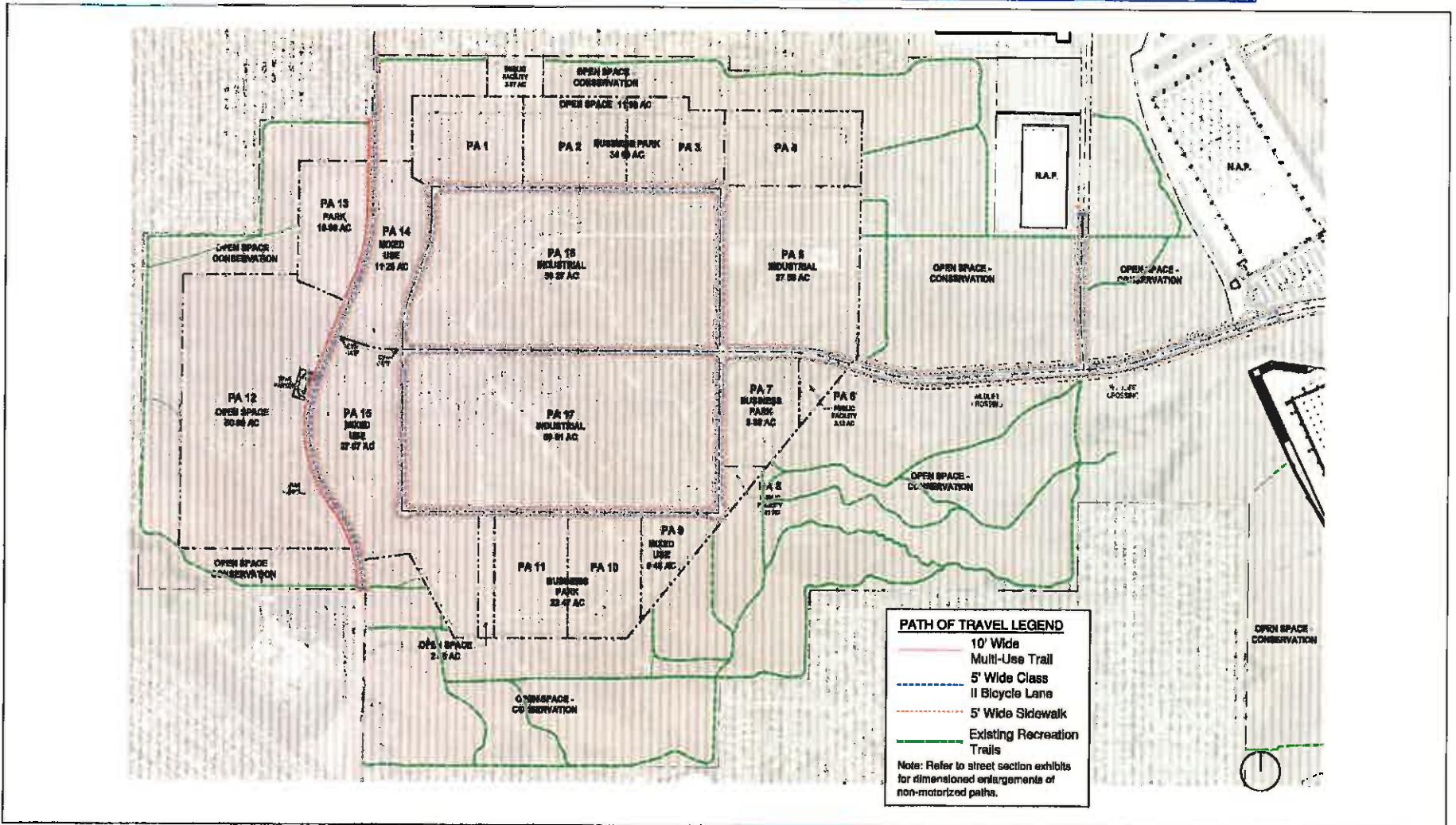


Source(s):

Figure 5-3

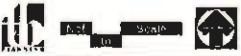


Truck Route



Source(s): Birch & Associates, Inc. (02-03-2022)

Figure 5-4



## 6 INFRASTRUCTURE AND GRADING

### 6.1 EXISTING INFRASTRUCTURE

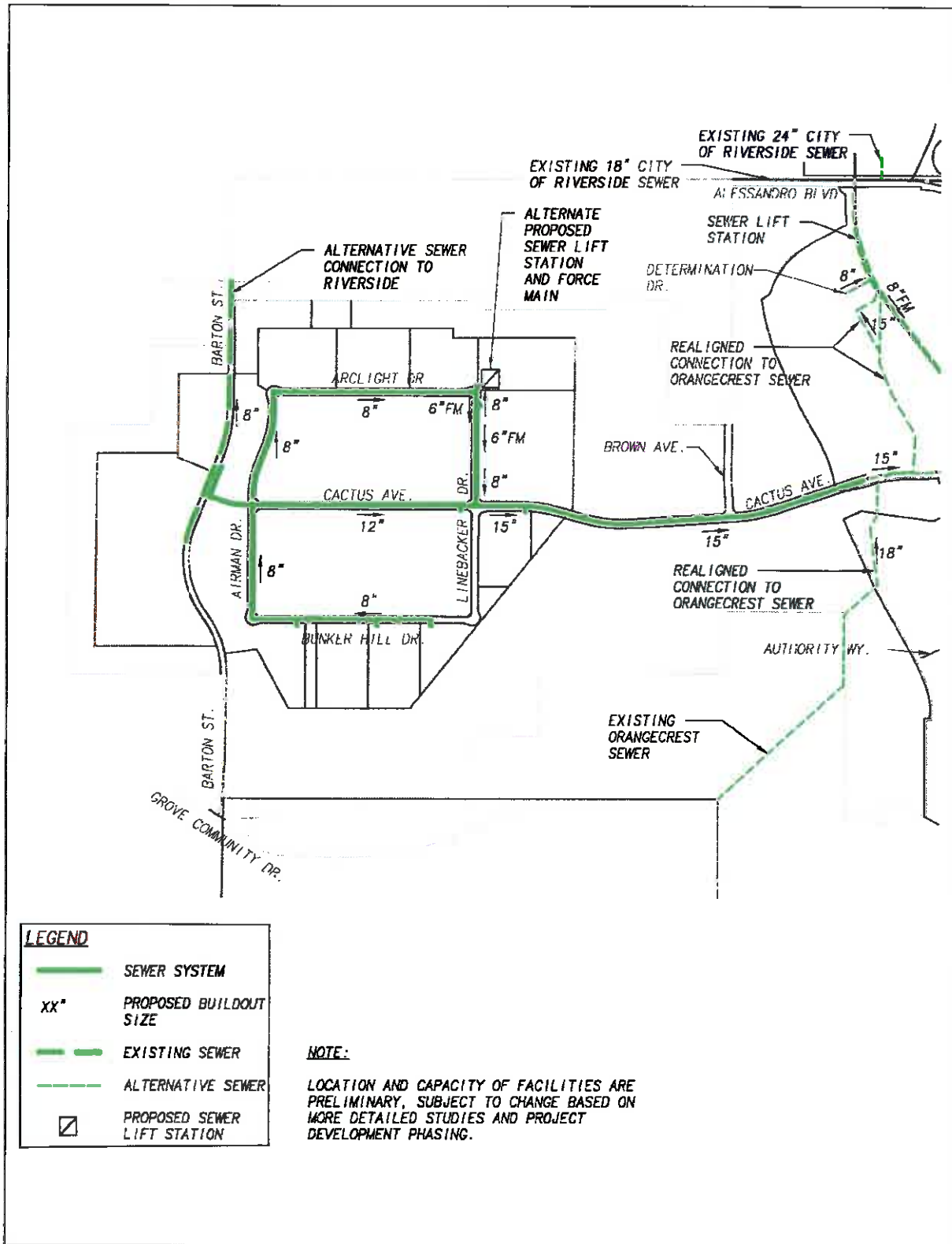
The West Campus Upper Plateau Specific Plan area consists of vacant land with minimal municipal utilities or services. Public facilities, services, and infrastructure will be provided concurrently with the appropriate phase of project development.

### 6.2 SEWER SERVICE AND FACILITIES

Sanitary sewer service for the Specific Plan Area is provided by the Western Municipal Water District (WMWD). Currently, existing 15-inch City of Riverside Sewer lines are located within the Cactus Avenue right-of-way to the east of the Specific Plan boundary, which connects to an 8-inch force main sewer line in Meridian Parkway. Existing 8-inch sewer lines are located within the Barton Street right-of-way to the northwest and southwest of the Specific Plan area in the City of Riverside. The proposed sewer service plan represents the primary sewer connection being provided via Cactus Avenue and flowing to the east. A sewer connection is also identified via Barton Street to the north to the City of Riverside Sewer system, which would serve as an alternative should the park and open space lot be infeasible to sewer to the east to Cactus due to grade elevations. *Figure 6-1, Sewer System*, shows sewer facilities improvements for the Specific Plan area.

The Specific Plan area requires the planning, design, and construction of the sewer systems, which include: installing a sewer lift station on the northeastern corner of Arclight Drive and Linebacker Drive (as needed dependent upon final grading and pad elevations); installing an 8-inch sewer line in Arclight Drive, Airman Drive, and Bunker Hill Drive; installing a 12-inch sewer line in Cactus Avenue east of Airman Drive and west of Linebacker Drive; installing a 15-inch sewer line in Cactus Avenue east of Linebacker Drive; and installing an 8-inch sewer line in Linebacker Drive north of Cactus Avenue to connect with the 15-inch sewer line in Cactus Avenue. The 15-inch WMWD sewer lines currently stubbed at the terminus of the Cactus Avenue cul-de-sac.





Source(s): DRC Engineering (01-22-2022)

Figure 6-1



Not to Scale

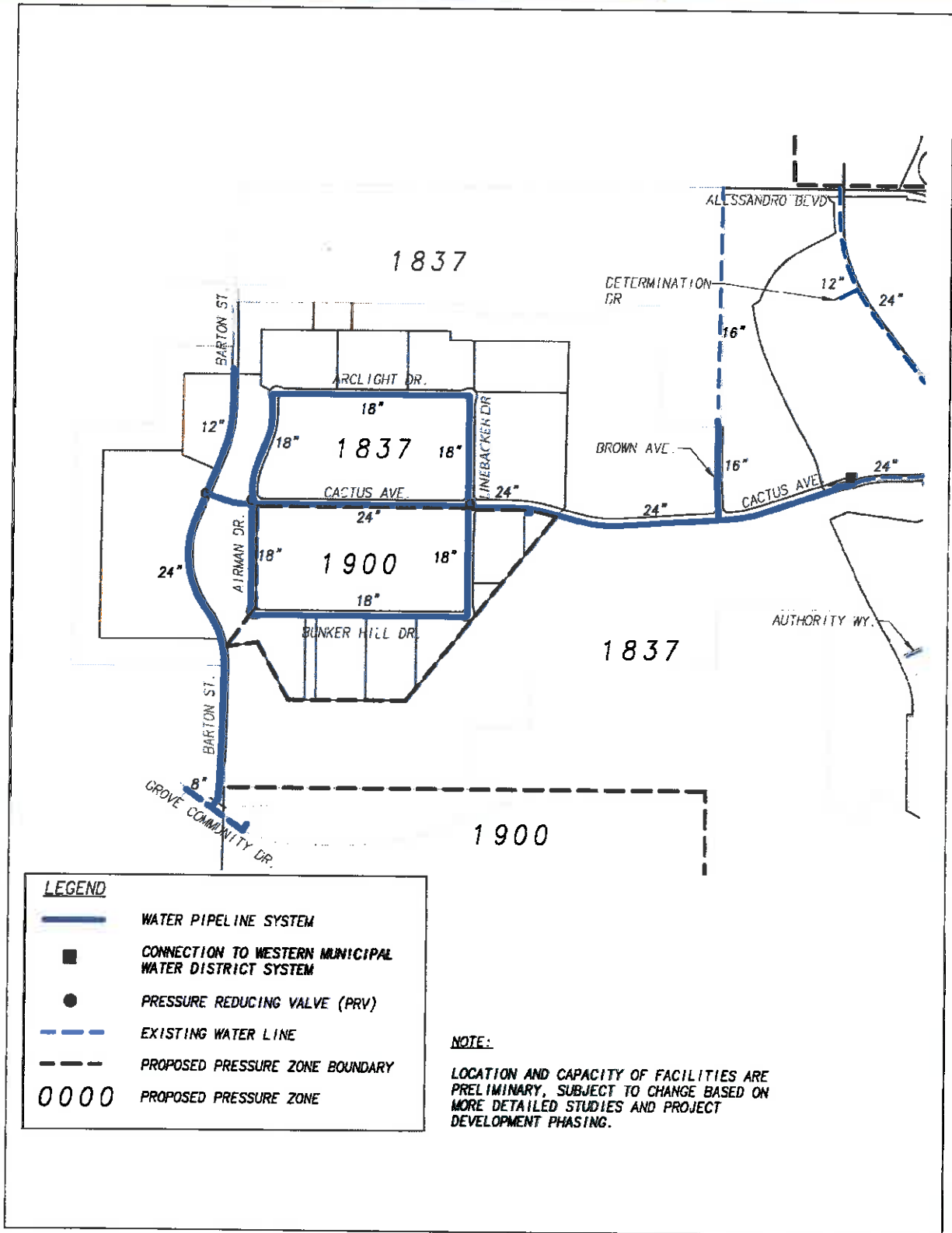


### 6.3 POTABLE WATER SERVICE

When March Air Force Base was an active-duty military installation, it consumed 2.14 million gallons of water a day for both domestic and irrigation uses. Potable water delivered to the Specific Plan Area is supplied by the Western Municipal Water District (WMWD) via a 24-inch distribution main operated by the WMWD. *Figure 6-2, Potable Water System*, illustrates project water supply facilities. All potable water facilities, including water mains, zone transitions, pressure pumps and reducers, storage facilities, will be operated and maintained by WMWD.

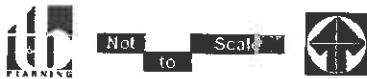
Currently, an existing 24-inch WMWD water line is located within the Cactus Avenue right-of-way to the east of the Specific Plan Area, and an existing 16-inch WMWD waterline is located within the Brown Avenue right-of-way. An 8-inch water line is located in Grove Community Drive to the south of the Specific Plan Area. The proposed potable water plan would connect to the 24-inch WMWD waterline via Cactus Avenue and loop to Barton Road.

The Specific Plan area requires the planning, design, and construction of the potable water systems, which include: installing pressure reducing valves (PRV) on the intersection of Linebacker Drive and Cactus Avenue, Airman Drive and Cactus Drive, and Barton Street and Cactus Drive; installing 18-inch water line in Arclight Drive, Airman Drive, Bunker Hill Drive, and Linebacker Drive; installing 12-inch water line in Barton Street north of Cactus Avenue; installing 24-inch water line in Barton Street south of Cactus Avenue; and installing 24-inch water line in Cactus Avenue. The 24-inch water line in Cactus Avenue will connect to the existing potable water facilities currently stubbed at the terminus of the Cactus Avenue cul-de-sac.



Source(s): DRC Engineering (01-22-2022)

Figure 6-2



## 6.4 RECLAIMED WATER

Reclaimed water service for the Specific Plan area will be provided by the Western Municipal Water District (WMWD). Existing 12-inch reclaimed water lines are located within the Cactus Avenue right-of-way to the east of the Specific Plan area. The proposed reclaimed water plan would connect to the existing Cactus Avenue service line. *Figure 6-3, Reclaimed Water System*, shows reclaimed water facilities improvement in the Specific Plan area.

The Specific Plan area requires the planning, design, and construction of the reclaimed water systems, which include the installing of 12-inch reclaimed water lines in Barton Street, Airman Drive, Arclight Drive, Cactus Avenue, Bunker Hill Drive, and Linebacker Drive. The 12-inch reclaimed water line in Cactus Avenue will connect to the existing reclaimed water facilities maintained by the WMWD.

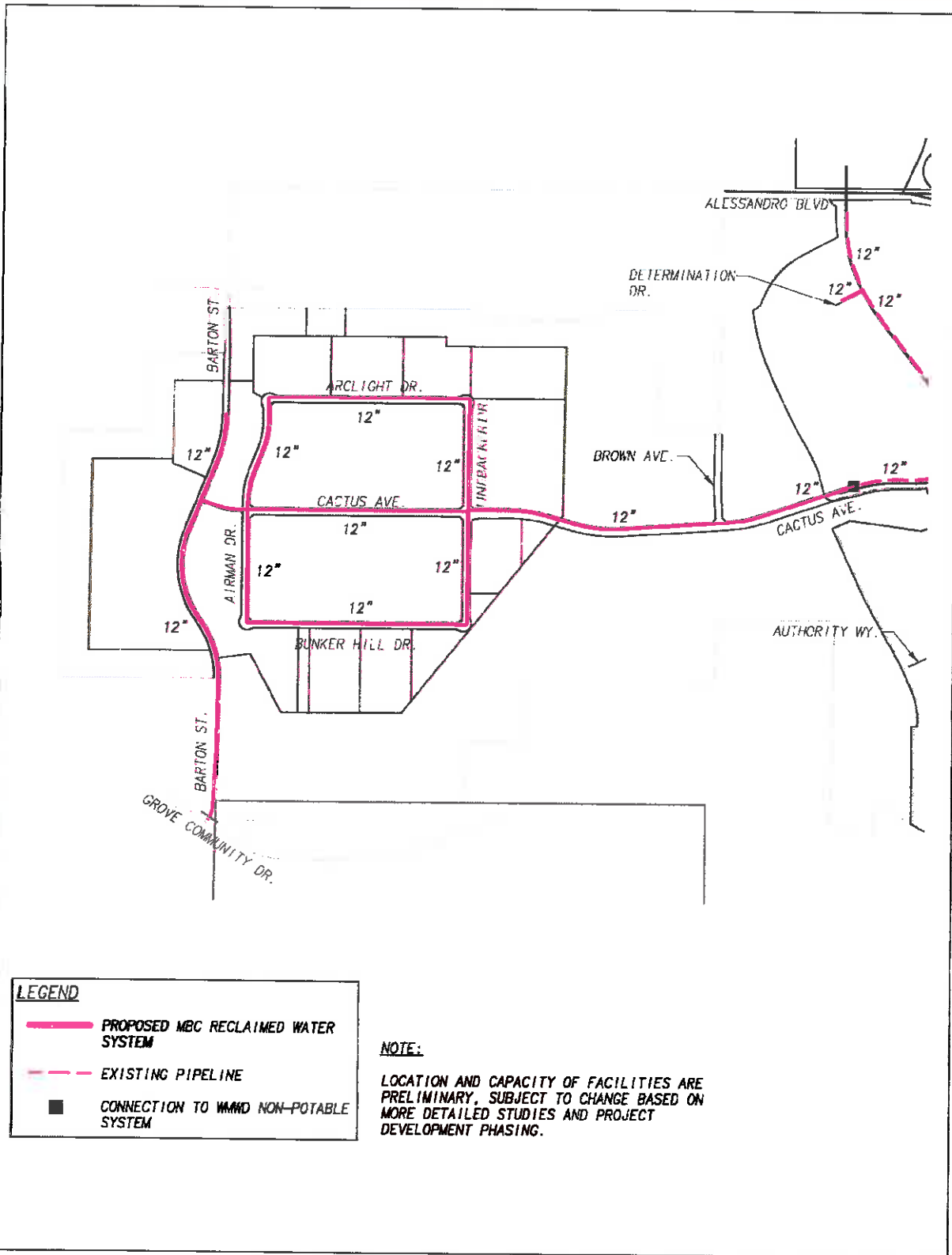
## 6.5 STORM WATER MANAGEMENT

The West Campus Upper Plateau storm water management plan is shown on *Figure 6-4, Storm Drain System*. Storm water in the northeastern portion of the Project area will be detained and ultimately flow to a detention basin on Alessandro Boulevard via an open channel. Storm water in the other parts of the Specific Plan Area will be detained and flow through a storm drain system and ultimately discharged to existing native flow lines across the boundary of the Specific Plan Area to match historical drainage patterns. Discharge points will be required to detain and mitigate flows to 90% of pre-developed flows to ensure there are no downstream erosion issues.

As indicated on *Figure 4-6*, various storm drain will in installed within the Specific Plan area. Storm drain improvement includes the following: a 24-inch storm drain system in the intersection of Cactus Avenue and Brown Avenue; a 24-inch storm drain system in Cactus Avenue east of Linebacker Drive and west of Brown Avenue; a 24-inch storm drain system in Cactus Avenue that stretches from Barton Street to the Public Facility land use on the eastern side of the Specific Plan area; a 24-inch storm drain system in Linebacker Drive south of Cactus Avenue; an 18-inch storm drain system that expands to 36-inches in Linebacker Drive north of Cactus Avenue extending to the northern side of the Specific Plan area; an 18-inch storm drain system that expands to 24-inches in Arclight Drive extending to the northern side of the Specific Plan Area; a 24-inch storm drain system in Airman Drive that extends to the northern side of the Specific Plan Area; a 24-inch storm drain system in Bunker Hill Drive extending to Airman Drive to the south of Cactus Avenue; and a storm drain system ranging from 18 to 36 inches in Barton Street extending to the northern, southern, and western border of the Specific Plan area. The storm drain system would ultimately connect with various open native channels and carry storm water off the Specific Plan Area consistent with historic drainage patterns.

## 6.6 GAS AND DRY UTILITIES

Southern California Gas Company and Southern California Edison will provide natural gas and electricity to the West Campus Upper Plateau Specific Plan area, respectively. As shown in *Figure 6-5, Gas Backbone*, *Figure 6-6, Electrical Backbone*, *Figure 6-7, Telephone Backbone*, and *Figure 6-8, Cable TV Backbone*, primary gas and dry utility lines will be installed to connect to existing gas and dry utility lines at Cactus Avenue to the east of the Specific Plan Area, and loop to Barton Street. Telephone/fiber service will be provided by Frontier Communications, and Cable TV will be provided by Spectrum Communications.



Source(s): DRC Engineering (01-22-2022)

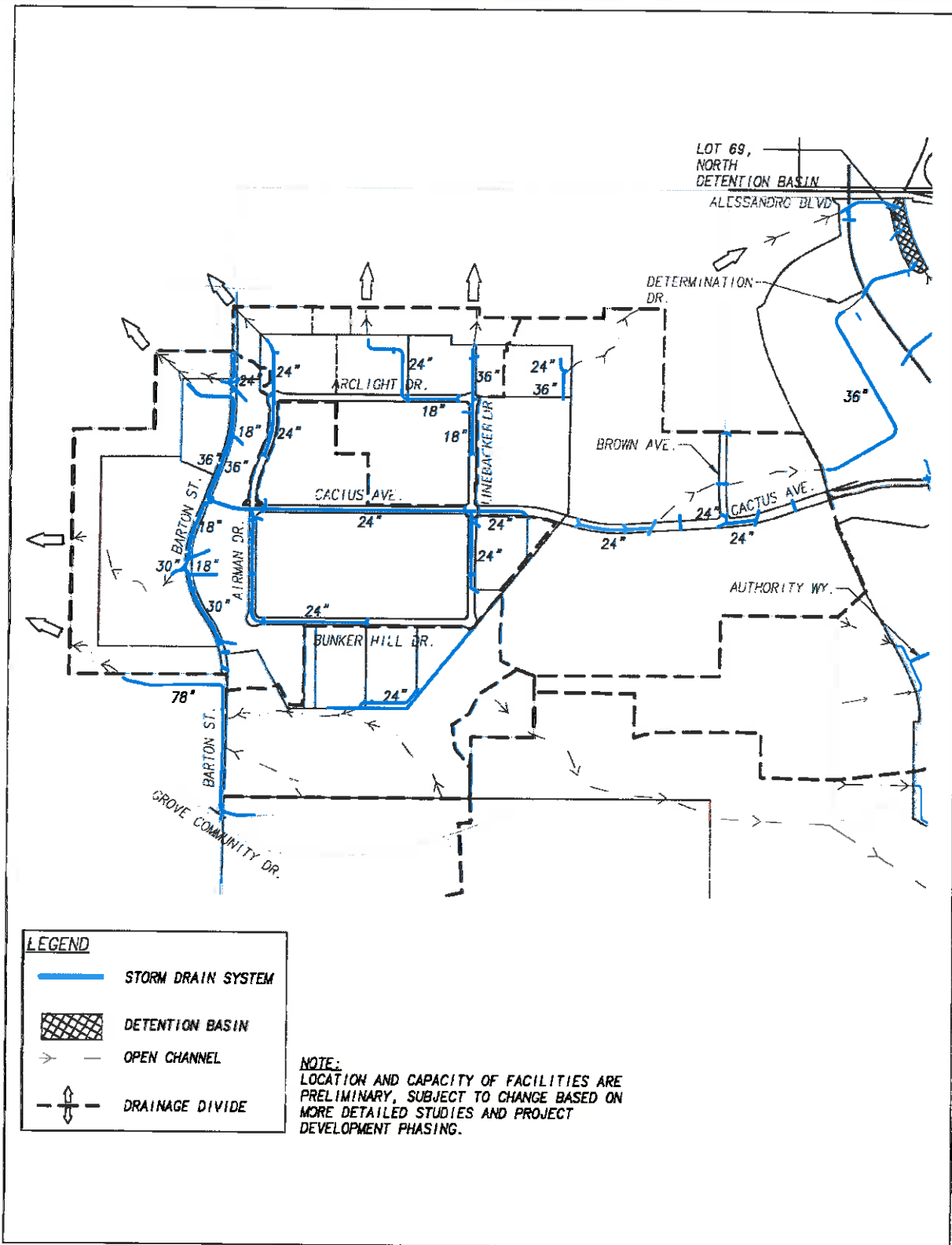
Figure 6-3



Not to Scale



Reclaimed Water System



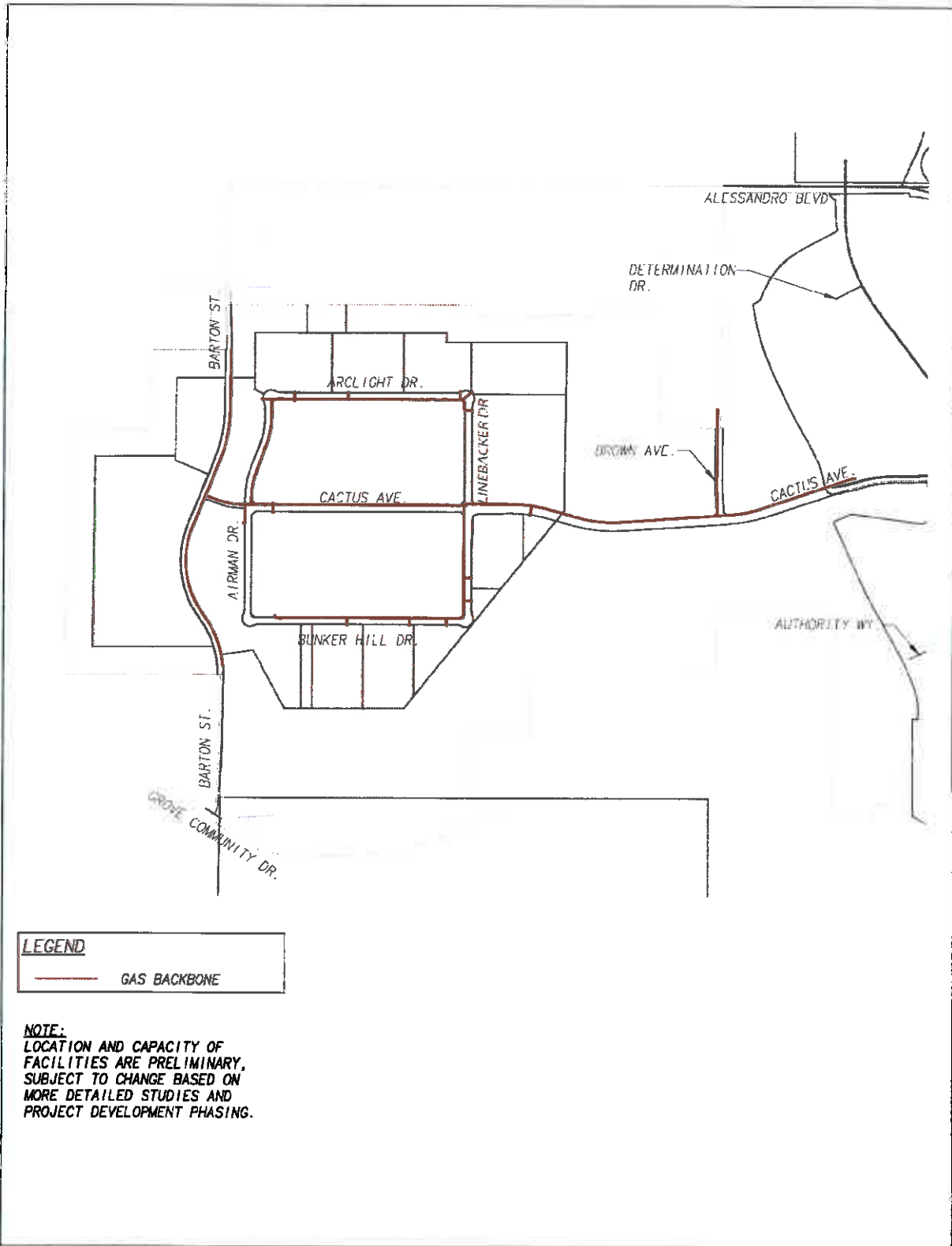
Source(s): DRC Engineering (01-22-2022)

Figure 6-4



Not to Scale





Source(s): DRC Engineering (01-22-2022)

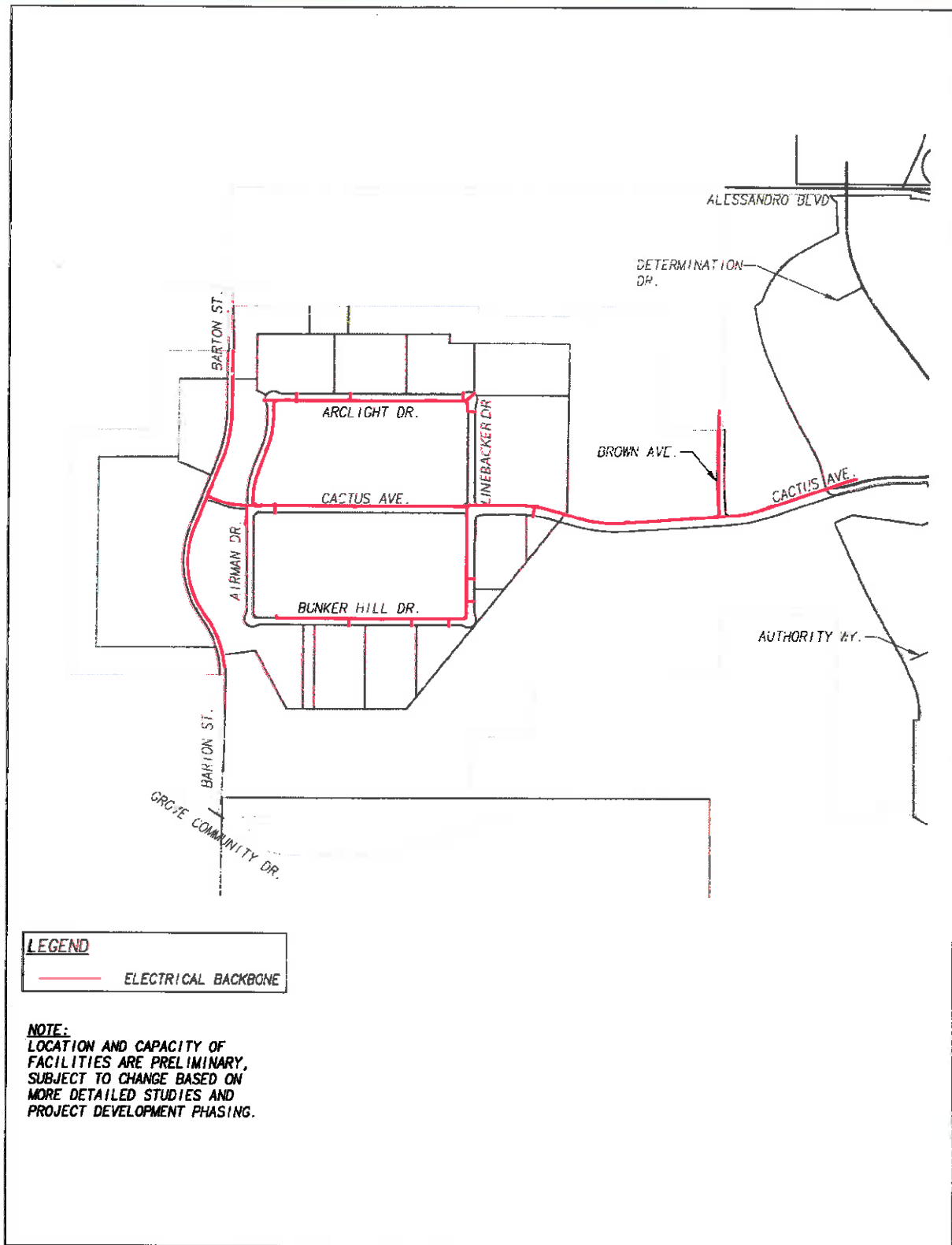
Figure 6-5



Not To Scale



Gas Backbone



Source(s): DRC Engineering (01-22-2022)

Figure 6-6

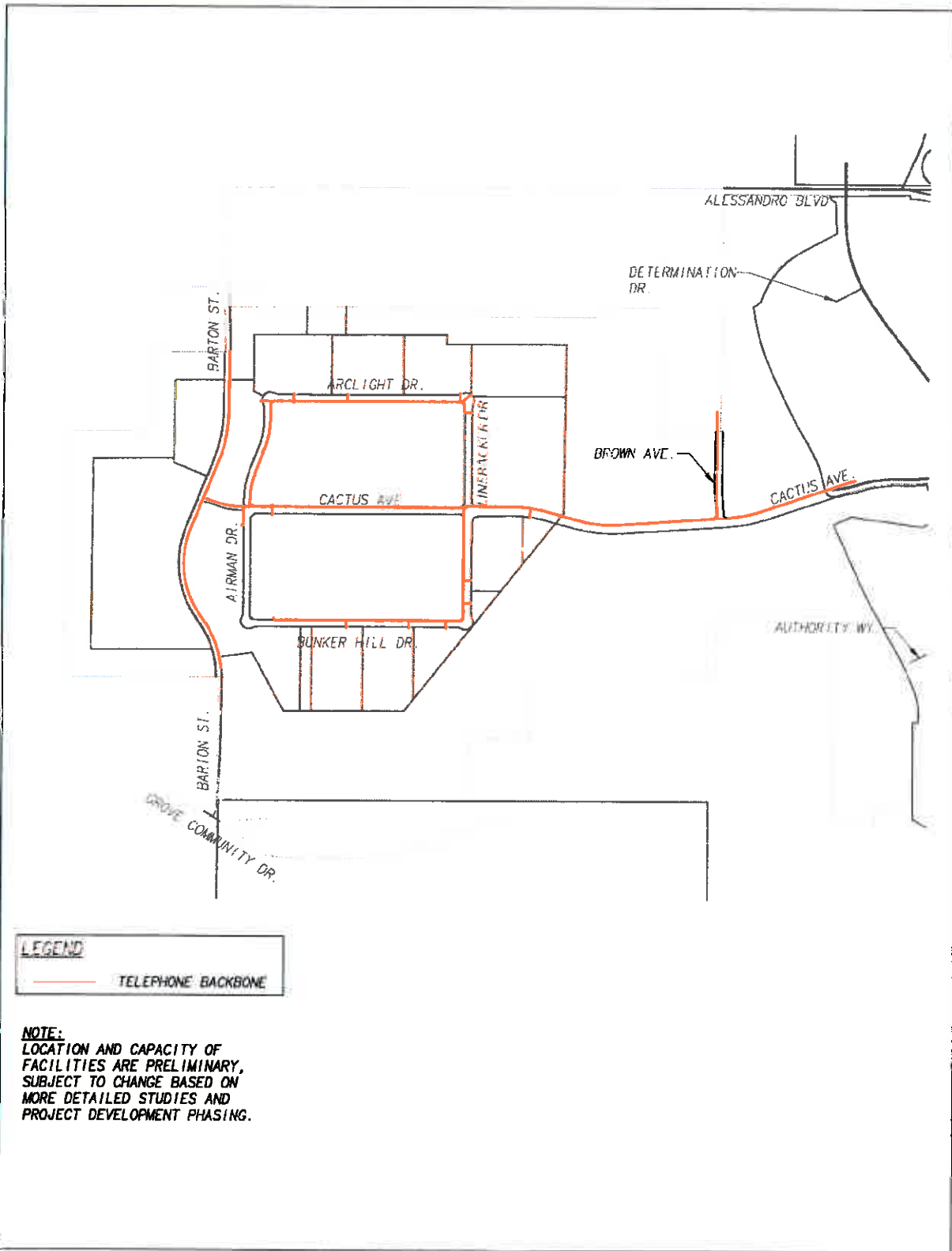


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### Electrical Backbone





Source(s): DRC Engineering (01-22-2022)

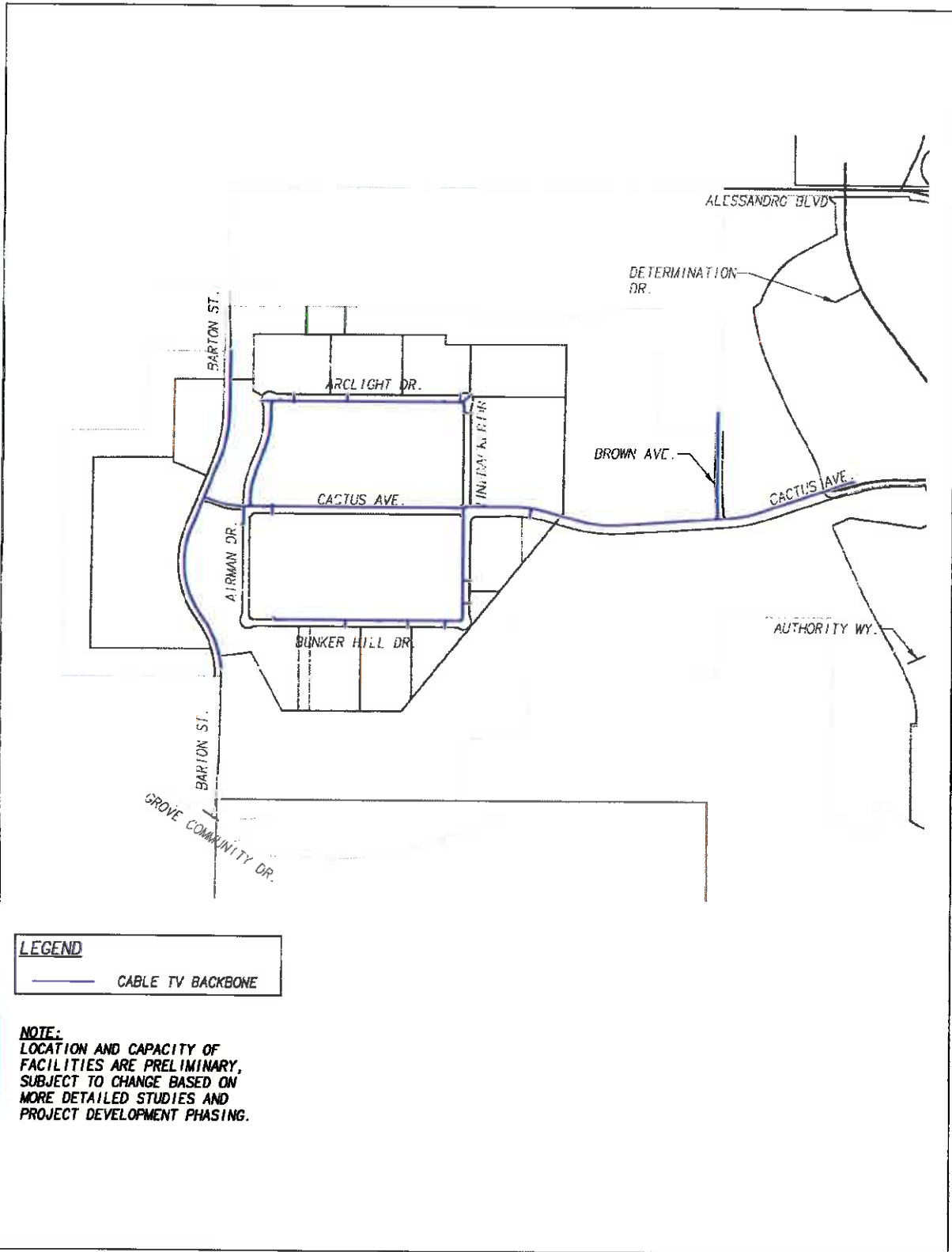
Figure 6-7



Not to Scale



Telephone Backbone



Source(s): DRC Engineering (01-22-2022)

Figure 6-8



Not to Scale



Cable TV Backbone

## 6.7 SOLID WASTE

Solid waste provider for the West Campus Upper Plateau Specific Plan is Burrtec Waste. Solid waste is disposed of at the El Sobrante, Lambs Canyon, and Badlands landfills. In order to reduce the amount of material generated by the Specific Plan, the West Campus Upper Plateau will comply with the requirements of the County of Riverside's Source Reduction and Recycling Element (SRRE).

## 6.8 GRADING

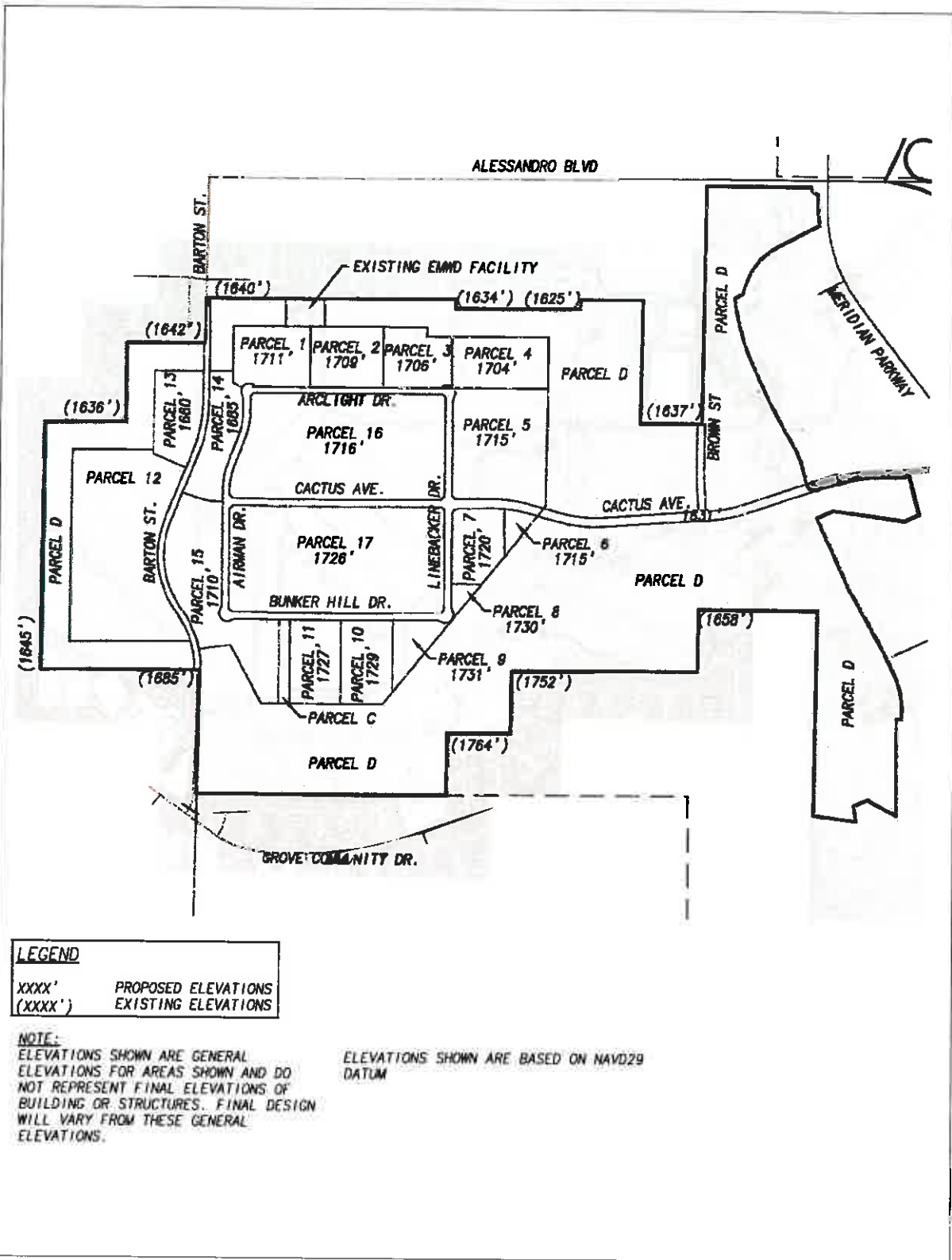
A conceptual grading design will be required for each Tentative Map application consistent with the March JPA Development Code. Grading designs will implement the goals and policies of the March JPA General Plan. *Figure 6-9, Conceptual Grading Exhibit*, shows the proposed grading for each individual parcel in the West Campus Upper Plateau Specific Plan area.

### 6.8.1 GRADING PLAN DEVELOPMENT STANDARDS

- All grading activities shall be in substantial conformance with the approved tentative map or development permit and shall implement any grading-related mitigation measures outlined in the accompanying EIR for the West Campus Upper Plateau.
- Prior to any development within any parcel of the Specific Plan, an overall grading plan for the portion in process shall be submitted for approval by the MJPA. The grading plan for each parcel shall be used as a guideline for subsequent grading plans for individual stages of development
- All streets shall have a gradient not exceeding use minimums and maximums established by the County of Riverside or as approved by the MJPA.
- A precise grading plan shall be prepared prior to any on-site grading for individual projects.
- The project developer/applicant shall be responsible for installation and maintenance of all planting and irrigation systems on manufactured slopes until those responsibilities are assumed by a Landscape Maintenance District or other parties.
- To the extent that is feasible, the overall shape, height, and gradient of any cut and fill slope shall be designed to be consistent with the existing natural contours and scale of the natural terrain.
- Potential brow ditches, terrace drains, or other minor swales, determined necessary at future stages of project review, shall be concealed, as feasible and possible, with landscape plantings, earth berms and similar features.
- Graded but undeveloped pads shall be maintained weed-free, appropriate erosion control measures within ninety (90) days of completion of grading, unless building permits are obtained from the MJPA. Appropriate desiltation basins are required for graded areas.
- Cut and fill slopes shall be constructed at inclinations of no steeper than two horizontal feet to one vertical foot, unless otherwise approved by the MJPA. Variable slope ratios will be used to avoid abrupt changes from the pads to the slopes.
- All newly created slopes exceeding 10 feet in vertical height shall be landscaped with a permanent irrigation system approved by the MJPA prior to final acceptance. Landscaping shall be consistent with the Specific Plan landscape design guidelines represented in Chapter 4.

- Grading shall not be permitted to commence prior to approval of grading permits for any proposed development. Mass grading will only occur for those areas undergoing development, or for those areas specifically identified as borrow or disposal sites.
- Grading operations within the confines of the Specific Plan area shall conform to all applicable MJPA Development Code standards.
- Project grading design shall make reasonable efforts to balance cut and fill on site to avoid the need for excessive importing or exporting of soil.

Manufactured slopes greater than 10 feet in vertical height, together with landscaping and irrigation systems, will be maintained by assessment district or owner's association. These slope areas will be entirely within a separate lot or easement. Irrigation systems maintained by the assessment district or owner's association will be separate from private systems. All slopes less than 10 feet in vertical height will be maintained by each project consistent with the MJPA Development Code.



Source(s):

Figure 6-9



Not to Scale



Conceptual Grading Exhibit

## 7 IMPLEMENTATION

### 7.1 SEVERABILITY

This Specific Plan document enables the March Joint Powers Authority (MJPA) to facilitate the processing and approval of development plans and implementing permits to build out the West Campus Upper Plateau Specific Plan area. If any regulation, condition, program, or portion of this Specific Plan is held invalid or unenforceable, such portions shall be deemed separate, distinct, and independent provisions, and the invalidity of such portions or provisions shall not affect the validity and enforceability of the remaining provisions contained herein.

### 7.2 APPLICABILITY

Approval of the West Campus Upper Plateau Specific Plan indicates acceptance by the MJPA of a general framework for the development of the Specific Plan property. Part of that framework establishes specific development standards that constitute the zoning regulations for the Specific Plan (refer to *Chapter 3, Development Regulations*). The provisions contained herein are intended to regulate development within the Specific Plan area.

Development within the West Campus Upper Plateau Specific Plan boundary shall be implemented through the MJPA's approval of tentative and final parcel maps and the Development Review process as established in the March Joint Power Authority's Development Code. The implementation process described herein provides the mechanisms for review and approval of development projects within the West Campus Upper Plateau Specific Plan.

### 7.3 INTERPRETATION

Unless otherwise provided, any ambiguity concerning the content or application of the Specific Plan shall be resolved by the MJPA's Planning Director, or his/her designee, in a manner consistent with the goals, policies, purpose, and intent established in this Specific Plan.

### 7.4 DEVELOPMENT REVIEW PROCESS

#### 7.4.1. Subdivision Maps

Approval of future tentative subdivision maps within the West Campus Upper Plateau Specific Plan may occur concurrently with or subsequently to the adoption of the Specific Plan. All tentative and final subdivision maps shall be reviewed and approved pursuant to the applicable provisions of the MJPA and consistent with the applicable provisions established within the Land Use, Infrastructure, Design Guidelines, and Development Regulations chapters of this Specific Plan.

#### 7.4.2. Development Plan Review

All development within the Specific Plan property shall be subject to the Development Review Process established in the MJPA's Development Code. Adoption of this Specific Plan by the Joint Powers Authority includes the design guidelines contained in Chapter 4, which shall be the design criteria by which development projects with the Specific Plan shall be reviewed during Development Plan Review. Topics on which these design guidelines are silent, the applicable design guidelines contained within the Authority's Development Code shall apply. The design guidelines are intended to be flexible in nature while establishing rudimentary evaluation criteria for the review by the Authority of development projects during design review.

#### **7.4.3. Conditional Use Permits**

Uses specified as conditionally permitted uses within Chapter 3, Development Regulations, of this Specific Plan shall be reviewed and approved by the MJPA pursuant to the requirements of the MJPA Development Code, Section 9.02.060, "Conditional Use Permits."

#### **7.4.4. Variances**

Administrative variances with respect to setback dimensions, lot coverage, building height, and fence height shall be reviewed pursuant to the Section 9.02.090 "Administrative Variances" of the MJPA Development Code. All other variances shall be considered in accordance with Section 9.02.100 "Variances" of the MJPA Development Code.

#### **7.4.5. Development Agreement**

Due to the scale and complexity of the Project, a Development Agreement is proposed to vest the Project entitlements and fees, ensure financing of public improvements required by the conditions of approval, and provide certain Community Benefits including compliance with the terms of the 2012 Settlement Agreement, and provision of new public benefits, including, but not limited to, expansion of employment opportunities for area residents.

### **7.5 SUBSTANTIAL CONFORMANCE**

All development under the Specific Plan is subject to a Substantial Conformance Determination, considered and approved ministerially by the Planning Director or designee. The Substantial Conformance Determination is also a mechanism that allows for the approval of ministerial minor modifications for development under the Specific Plan. The MJPA recognizes that modifications to the text and exhibits of this document may be needed over time. Upon direction by the MJPA Planning Department, certain modifications to text, exhibits, and/or development standards and design guidelines may not require a formal Specific Plan Amendment (i.e., through public hearing) and occur ministerially. The following minor modifications to this document do not require a formal Specific Plan Amendment and are subject to review and approval by the Planning Director. The Planning Director shall have the discretion to defer any request for modification to the March Joint Powers Commission, either for ministerial direction and guidance, or determination that the required change requires a formal amendment as discussed below in Section 7.6. Ministerial substantial conformance decisions are not subject to CEQA compliance.

- Expansions or reductions of the net acreage covered by a given Planning Area.
- A decrease in development intensity/density (building square footage).
- Modification of design criteria such as architectural details, landscape treatments, fencing, lighting, and entry treatments.
- Changes to the Phasing Plan, provided infrastructure is available to serve the phase.
- Implementation of alternative landscape materials, wall and fence materials, entry monument design, corner treatments, and streetscape design that are generally consistent with the conceptual design guidelines contained within this Specific Plan.
- Modifications to Architectural Design Guidelines, such as variation of architectural style and variations in materials and colors.
- Final infrastructure facility sizing, and precise location of dry utilities, water, sewer, and storm drainage improvements as approved by the Planning Director or applicable utility agency.
- Roadway ROW design when the changes are warranted and approved by the Planning Director.

- Revisions to exhibits which do not substantially change the intent of the Specific Plan.
- Modification, deletions, and additions to the list of permitted and conditional uses.
- Specific modifications of a similar nature to those listed above which are deemed minor by the Planning Director, which are in keeping with the intent of this Specific Plan and which are in conformance with the MJPA General Plan.

## 7.6 FORMAL AMENDMENTS TO THE SPECIFIC PLAN

All modifications to this document which do not meet the criteria of a Substantial Conformance (as defined in the previous subsection) shall be deemed to require a formal Specific Plan Amendment. This document was prepared pursuant to California Government Code §65450, et. seq. Amendments shall be processed in accordance with the applicable requirements of the law, which include §65450, et. seq. of the California Government Code.

Formal Specific Plan Amendments shall be subject to the review and approval of the March Joint Powers Commission. As required by the California Government Code, all government agencies significantly affected by the proposed Amendment shall be notified of the proposed action prior to the approval. In addition, and as required by CEQA, formal Specific Plan Amendments shall be appropriately reviewed in accordance with the State CEQA Guidelines.

Any formal Specific Plan Amendment initiated by an applicant requires preliminary review by the Planning Director, filing of an official application and required materials supporting the Amendment, submittal of a fee deposit, and March Joint Powers Commission review and final decision.

## 7.7 APPEALS

Appeals of any determination of the Planning Director may be made by the applicant or any other aggrieved party by filing an application on forms provided by the MJPA and accompanied by the appropriate filing fee, where applicable, within ten (10) days following the final date of action for which an appeal is made. Appeals shall be processed consistent with the provisions of Section 9.02.240 "Appeals" of the MJPA Development Code.

## 7.8 COMPLIANCE WITH MITIGATION MONITORING AND REPORTING PROGRAM

Certification of an EIR shall be required prior to the approval of the Specific Plan. Development within the West Campus Upper Plateau Specific Plan shall comply with all approved mitigation measures as described in the MMRP included as part of the EIR.

The financing of construction, operation, and maintenance of public improvements, facilities, and public services shall include funding through a combination of financing mechanisms. Prior to the recordation of final maps, a final determination shall be made by MJPA staff and confirmed by the Planning Director and MJPA Engineer regarding the responsibility for construction and maintenance of public facilities, whether publicly or privately maintained.

Implementation of the West Campus Upper Plateau Specific Plan may involve financing options including, but not limited to, the following:



**7.8.1 Facilities and Services**

Construction of public improvements and facilities and the provision of public services may be financed through private capital investment, a Community Facilities District (CFD), or other special district, pursuant to the Mello-Roos Community Facilities District Act of 1982.

**7.8.2 Operation and Maintenance**

Options for operation and maintenance of public improvements and facilities include, but are not limited to, the following:

- Individual private property owners
- Private Property Owners Association
- LLMD (Landscaping and Lighting Maintenance District) or CFD (will be at the JPA's sole discretion)

**7.9 MAINTENANCE PLAN**

The public and private improvements constructed within the West Campus Upper Plateau Specific Plan shall be maintained through a combination of public and private entities as described in *Table 7-1, Maintenance Responsibilities*. Table 7-1 provides a list of maintenance entity options that may fund and/or maintain facilities within the Specific Plan. A Property Owners Association (POA) shall be established for the maintenance of common area landscape improvements and private roadways within areas of the Specific Plan. For areas in public ownership (such as public roadway ROWs), municipal maintenance districts may fund the maintenance of these areas.

**Table 7-1 Maintenance Responsibilities**

Facility	MJPA and/or CFD/LLMD	Property Owners Association	Property Owner or Occupant
Roadways (Cactus Avenue, Barton Street, Airman Drive, Arcflight Drive, Bunker Hill Drive)			
Curb-to-curb improvements	✓		
Parkways within public right-of-way (ROW) or LLMD	✓		
Neighborhood Edges, Master Plan Trails and Medians	✓		
Landscape buffer located outside public ROW/LLMD		✓	
Off-street parking areas			✓
Traffic control signs – in the public ROW/LLMD	✓		
Traffic control signs – not in the public ROW/LLMD		✓	
Streetlights – in the public ROW	✓		
10-acre public park	✓		
Streetlights – not in the public ROW/LLMD		✓	
Public water, sewer, and storm drain improvements within public ROW (excluding laterals)	✓		
On-site landscaping and irrigation		✓	✓
Common open space		✓	
Walls and fences		✓	✓
Corner and Entry Monuments		✓	

Facility	MJPA and/or CFD/LLMD	Property Owners Association	Property Owner or Occupant
Tenant Signage		✓	✓
Off-street lighting		✓	✓
Offsite Storm Water Drainage/Water Quality Facilities – within the public ROW/LLMD	✓		
Onsite Storm Water Drainage/Water Quality Facilities (swales, basins, biotreatment filters, etc)		✓	
Fiber Optic communication system in the public ROW	✓		

## 8 CONSISTENCY WITH THE GENERAL PLAN

### 8.1 OVERVIEW

The West Campus Upper Plateau Specific Plan is based upon the goals and policies set forth in the March JPA General Plan. This section addresses conformance of the West Campus Upper Plateau Specific Plan to the General Plan on a general or conceptual basis.

### 8.2 GENERAL PLAN ELEMENTS

#### 1. Land Use

**Goal 1:** *Land Use Plan provides for a balanced mix of land uses that contribute to the regional setting, and capitalize on the assets of the Planning Area, while insuring compatibility throughout the Planning Area with the regional plans.*

**Consistency:** Development of the West Campus Upper Plateau will occur in a logical pattern of growth, compatible with adjacent land uses and regional plans. The project will provide an employment center in a portion of the County that is largely residential. This will improve the balance of population and employment in the project vicinity, providing opportunity for residents to work locally, rather than commute to surrounding areas and counties.

**Goal 2:** *Locate land uses to minimize land use conflict or creating competing land uses and achieve maximum land use compatibility while improving or maintaining the desired integrity of the Planning Area and subregion.*

**Consistency:** The land use summary in Table 3-1 provides a mixture of compatible land uses that may be developed in the Specific Plan area. Incompatible or competing land uses will not be allowed in the Specific Plan area.

**Goal 3:** *Manage growth and development to avoid adverse environmental and fiscal effects.*

**Consistency:** Development of the project will be phased to the assurance of required infrastructure and services.

**Goal 4:** *Develop an identity and foster quality development within the Planning Area.*

**Consistency:** The West Campus Upper Plateau Design Guidelines establish architectural, signage, parking, and landscaping standards that will develop a project identity and foster quality development.

**Goal 5:** *Maximize and enhance the tax base and generation of jobs through new, reuse and joint use opportunities.*

**Consistency:** The West Campus Upper Plateau will be an employment center providing substantial enhancement to the tax base.

**Goal 6:** *Support the continued Military Mission of March Air Reserve Base, and preservation of the airfield from incompatible land use encroachment.*

**Consistency:** The industrial, mixed use, and recreational activities anticipated from development of the project site will be consistent with other existing uses within the March JPA boundary. These uses will support the Military Mission of the March ARB.

**Goal 7:** *Maximize the development potential as a regional Intermodal Transportation facility to support both passenger and freight-related air services.*

**Consistency:** The West Campus Upper Plateau will develop warehouse, storage, and distribution facilities that can serve as a regional Intermodal Transportation facility to support passenger and freight-related air services.

**Goal 8:** *Preserve the natural beauty, minimize degradation of the March JPA Planning Area, and provide enhancement of environmental resources, and scenic vistas.*

**Consistency:** The West Campus Upper Plateau will preserve approximately 445 acres of open space surrounding the Specific Plan Area, preserving environmental resources and scenic vistas of the March JPA Planning Area.

**Goal 9:** *Preserve the integrity of the historic and cultural resources of the Planning Area and provide for their enhancement.*

**Consistency:** Most of the project site previously served as the ammunition storage area for the former March AFB. The West Campus Upper Plateau Specific Plan proposes to embrace this more recent historic use of the site by preserving one of the numerous ammunition bunkers located on the project site.

**Goal 10:** *Avoid undue burdening of infrastructure, public facilities, and services by requiring new development to contribute to the improvement and development of the March JPA Planning Area.*

**Consistency:** Future development within the West Campus Upper Plateau Specific Plan project area will be required to develop infrastructure and public facilities, as well as provide fair share financial contributions necessary to ensure no adverse impact or undue burdening to public infrastructure or services occur.

**Goal 11:** *Plan for the location of convenient and adequate public services to serve the existing and future development of March JPA Planning Area.*

**Consistency:** All public facility connections are located adjacent to the site, and adequate capacity has been deemed available by the responsive agencies. Service facility letters were obtained from these agencies and their comments/recommendations have been incorporated into the project accordingly.

**Goal 12:** *Ensure, plan, and provide adequate infrastructure for all facility reuse and new development, including but not limited to, integrated infrastructure planning, financing, and implementation.*

**Consistency:** Development of the project will be phased to the assurance of required infrastructure and services. The Specific Plan accommodates a number of financing strategies to fund public improvement.

**Goal 13:** *Secure adequate water supply system capable of meeting normal and emergency demands for existing and future land uses.*

**Consistency:** As described in Chapter 6, *Infrastructure and Grading*, the water supply system will have sufficient capacity to accommodate projected normal and emergency needs.

**Goal 14:** *Establish, extend, maintain, and finance a safe and efficient wastewater collection, treatment and disposal system which maximizes treatment and water recharges, minimizes water use, and prevents groundwater contamination.*

**Consistency:** As described in Chapter 6, *Infrastructure and Grading*, the West Campus Upper Plateau will provide the necessary facilities to establish a wastewater collection, treatment, and disposal system.

**Goal 15:** *In compliance with state law, ensure solid waste collection, siting, and construction of transfer and/or disposal facilities, operation of waste reduction and recycling programs, and household hazardous waste disposal programs and education are consistent with the County Solid Waste Management Plan.*

**Consistency:** Development within the West Campus Upper Plateau Specific Plan area will comply with the requirements of the County of Riverside's Source reduction and Recycling Element (SRRE).

**Goal 16:** *Adequate supplies of natural gas and electricity from utility purveyors and the availability of communications services shall be provided within the March JPA Planning Area.*

**Consistency:** All public facilities connections are located adjacent to the site, and adequate capacity has been deemed available by the responsive agencies. Service facility letters were obtained from these agencies and their comments/recommendations have been incorporated into the project accordingly.

**Goal 17:** *Adequate flood control facilities shall be provided prior to, or concurrent with, development in order to protect the lives and property within the March JPA Planning Area.*

**Consistency:** As described in Chapter 6, *Infrastructure and Grading*, the West Campus Upper Plateau will provide drainage facilities to provide adequate flood control.

## **2. Transportation**

**Goal 1:** *Establish and provide for a comprehensive transportation system that captures the assets and opportunities of the planning area, existing transportation facilities, and planned transportation facilities for the future growth and development of the planning area and sub-region.*

**Consistency:** The West Campus Upper Plateau Specific Plan identifies and proposes to develop a comprehensive street network that will seamlessly connect and integrate with the existing roadway network and transportation improvements planned for the area and region.

**Goal 2:** *Build and maintain a transportation system which capitalizes on the multi-faceted elements of transportation planning and systems, designed to meet the needs of the planning area, while minimizing negative effects on air quality, the environment and adjacent land uses and jurisdictions.*

**Consistency:** The West Campus Upper Plateau Specific Plan proposed the development of a multi-faceted transportation network, providing functional and convenient vehicular

circulation, ease of access and use by pedestrians and cyclists, as well as utilizing existing and future transit opportunities afforded by the Riverside Transit Authority and Metrolink.

**Goal 3:** *Develop a transportation system that is safe, convenient, efficient and provides adequate capacity to meet local and regional demands.*

**Consistency:** The West Campus Upper Plateau will construct an internal street network and provide transportation capacity improvement to existing facilities off-site based on future demand. Transportation improvements will be constructed in phases based on project development and projected background traffic growth.

**Goal 4:** *Provide a balanced transportation system that ensures the safe and efficient movement of people and goods throughout the planning area, while minimizing the use of land for transportation facilities.*

**Consistency:** The West Campus Upper Plateau internal streets are sized to accommodate projected future vehicular traffic in a safe and efficient manner.

**Goal 5:** *Plan and encourage land use patterns and designs which enhance opportunities for non-vehicular circulation and improve trip reduction strategies.*

**Consistency:** Site plans for individual buildings shall be reviewed to ensure that pedestrian, bicycle and transit access is facilitated. A bicycle and pedestrian circulation network is planned for the project area and will be provided.

**Goal 6:** *Establish vehicular access control policies in order to maintain and insure the effectiveness and capacity of arterial roadways.*

**Consistency:** The project's internal roadways will be designed in accordance with the "County Road Improvement Standards and Specifications," published by the County of Riverside, and take into account additional requirements established in the Riverside County Integrated Plan.

**Goal 7:** *Facilitate and develop transportation demand management and transportation systems management programs, and use of alternate transportation modes.*

**Consistency:** Transportation Demand Management (TDM) strategies will be implemented to shift trips outside the standard commuting hours and/or to non- "drive alone" modes of travel. This is accomplished through various employer-initiated measures, such as flexible working hours, encouragement of carpooling, and facilitating access for non-motorized (i.e., bicycling or walking) modes of travel.

**Goal 8:** *Adequate, affordable, equitably distributed and energy efficient public and mass transit services which promote the mobility to, from, and within the planning area shall be provided.*

**Consistency:** The project area is served by the Riverside Transit Authority. Current bus service is adjacent to the site. In addition, the Moreno Valley Metrolink station is approximately 1.5 miles from the industrial campus portion of the project site. Future development will include improvements to encourage and support bus service within the project, particularly in the industrial campus area.

**Goal 9:** *Develop measures which will reduce the number or vehicle-miles traveled during peak travel periods.*

**Consistency:** The West Campus Upper Plateau will improve job/housing balance by providing an employment center in an area that is largely residential. This will allow residents to work locally, rather commuting to Los Angeles or Orange Counties. Job/housing balance will help reduce vehicle miles of travel.

**Goal 10:** *Regulate the travel of trucks on March JPA Planning Area streets.*

**Consistency:** The project is designed to accommodate truck traffic. In addition, trucks will be required to travel on designated routes as they traverse the West Campus Upper Plateau internal streets and connect to the

**Goal 11:** *Adequate off-street parking for all land uses shall be provided which requires adequate on-site parking to prevent spill over on the adjacent street system.*

**Consistency:** The West Campus Upper Plateau Specific Plan provides parking ratios that will limit the potential for parking spillover.

**Goal 12:** *Plan for and seek to establish an area-wide system of bicycling trails, with linkages within the planning area and adjacent jurisdictions, and in compliance with sub-regional plans.*

**Consistency:** The West Campus Upper Plateau Specific Plan roadway network will provide for bicycle lanes on all streets, as well as establish a 10-foot-wide multi-purpose trail along the west side of Barton Street. This will be of benefit to the employees and visitors to the Specific Plan, as well as provide connectivity for an improved local and regional bicycling network.

**Goal 14:** *Goods movement through the San Jacinto Rail Branchline shall be capitalized.*

**Consistency:** The West Campus Upper Plateau is located west of the BNSF railway, and goods movement within the Specific Plan Area can utilize the San Jacinto Rail Branchline.

**Goal 15:** *In accordance with state and federal law, promote and provide mobility for the disabled.*

**Consistency:** Development plans and public improvement plans shall take into account and ensure compliance with all applicable accessibility requirements of the Americans with Disability Act (ADA).

### **3. Noise/Air Quality**

**Goal 1:** *Ensure that land uses are protected from excessive and unwanted noise.*

**Consistency:** Project development shall be consistent with the land uses limitations established in the AICUZ study and the Riverside County Airport Land Use Plan.

**Goal 2:** *Minimize incompatible noise level exposures throughout the Planning Area, and where possible, mitigate the effect of noise incompatibilities to provide a safe and healthy environment.*

**Consistency:** Project development shall be consistent with the land uses limitations established in the AICUZ study and the Riverside County Airport Land Use Plan.

**Goal 3:** *Work toward the reduction of noise impacts from vehicular traffic, and aviation and rail operations.*

**Consistency:** The project shall implement all noise mitigation provisions established in the project EIR.

**Goal 4:** *Promote alternative modes of travel.*

**Consistency:** The transportation network that will be developed to serve the project will include improvements to encourage pedestrian and bicycle activity, as well as use of transit opportunities being provided by the Riverside Transit Authority and Metrolink.

**Goal 5:** *Reduce emissions associated with vehicle miles traveled by enhancing the jobs/housing balance of the subregion of western Riverside County.*

**Consistency:** The West Campus Upper Plateau improve job/housing balance in Western Riverside County by providing an employment center in an area that is largely residential. This will provide an opportunity for residents to work locally, rather than commute to Los Angeles or Orange Counties. Job/housing balance will help reduce vehicle miles of travel, resulting in reduced emissions.

**Goal 6:** *Reduce air pollution through proper land use, transportation and energy use planning.*

**Consistency:** The West Campus Upper Plateau Specific Plan will provide access using a variety of transportation modes, including bicycle and pedestrian activity. The project's Transportation Demand Management (TDM) strategy will accommodate the shift of some trips from "drive-alone" to transit or non-motorized modes of travel.

**Goal 7:** *Pursue reduced emissions for stationary and mobile sources through the use and implementation of new and advancing technologies.*

**Consistency:** Where feasible and appropriate, development of West Campus Upper Plateau shall accommodate the use of advancing technologies, such as alternate fueled vehicles and other innovations that would provide air quality benefits.

**Goal 8:** *Maximize the effectiveness of air quality control programs through coordination with other governmental entities*

**Consistency:** Development in the West Campus Upper Plateau will comply with the policies outlined in the March JPA General Plan, including compliance with the South Coast Air Quality Management District, improvement of the air quality in South Coast Air Basin, and cooperation with neighboring jurisdictions.

**Goal 9:** *Reduce emissions associated with vehicle/engine use.*

**Consistency:** The West Campus Upper Plateau improve jobs/housing balance in western Riverside County by providing an employment center in an area that is largely residential. This will provide an opportunity for residents to work locally, rather than commute to Los Angeles or Orange Counties. Job/housing balance will help reduce vehicle miles of travel. In addition, site improvements are encouraged that provide opportunity for utilization of electric vehicles.

**Goal 10:** *Reduce emissions associated with energy consumption.*



**Consistency:** Development in the West Campus Upper Plateau will comply with the policies outlined in the March JPA General Plan, including the use of the energy-efficient equipment and design, implementation of energy conservation features and recycling programs, and support of drought-resistant vegetation.

**Goal 11:** *Reduce Air pollution emissions and impacts through siting and building design.*

**Consistency:** Development in the West Campus Upper Plateau will comply with the policies outlined in the March JPA General Plan, including the use of low polluting construction materials and coatings and separation of sensitive receptors from toxic and carbon monoxide emissions.

**Goal 12:** *Reduce fugitive dust and particulate matter emissions.*

**Consistency:** Development in the West Campus Upper Plateau will comply with the policies outlined in the March JPA General Plan, including the implementation of fugitive dust reduction techniques, support of efficient street cleaning equipment, maintenance of the natural topography, and compliance with the South Coast Air Quality Management District.

#### 4. Housing

The March JPA General Plan did not identify any housing opportunities within the March JPA Planning Area due to land use compatibility issues related to the continued military activities of the Air Force Reserves and aviation operations. The General Plan also identifies the need to focus on the reestablishment of jobs lost due to base realignment, and the housing rich environment of Western Riverside County. The West Campus Upper Plateau Specific Plan maintains consistency with the General Plan's absence of a residential land use designation within the Planning Area.

#### 5. Resource Management

**Goal 1:** *Conserve and protect surface water, groundwater, and imported water resources.*

**Consistency:** The project will be constructed to minimize impacts to the existing drainage channels. The landscape plan includes drought tolerant plants. Irrigation will be moisture sensitive to limit irrigation during times of heavy rain.

**Goal 2:** *Control flooding to reduce major losses of life and property.*

**Consistency:** The West Campus Upper Plateau Specific Plan includes future improvements that properly capture, control, and maintain stormwater as required by state, regional, and local standards. These improvements will help control flooding

**Goal 3:** *Conserve and protect significant landforms, important watershed areas, mineral resources and soil conditions.*

**Consistency:** The project EIR has been prepared to assess and, if appropriate, mitigate project impacts upon geology, soils, and hydrology.

**Goal 4:** *Conserve energy resources through use of available energy technology and conservation practices.*

**Consistency:** The West Campus Upper Plateau shall comply with applicable regulations relating to energy conservation.

**Goal 5:** *Conserve and protect significant stands of mature trees, native vegetation, and habitat within the planning area.*

**Consistency:** The Specific Plan will preserve approximately 445 acres of open space surrounding the Specific Plan Area, conserving and protecting significant stands of habitat within the March JPA planning area.

**Goal 6:** *Provide an effective and efficient waste management system for solid and hazardous wastes that is financially and environmentally responsible.*

**Consistency:** The West Campus Upper Plateau shall comply with appropriate and applicable regulations and standards with respect to the management of solid and hazardous waste.

**Goal 7:** *Promote cultural awareness through preservation of the planning area's historic, archaeological, and paleontological resources.*

**Consistency:** The project EIR has been prepared to assess and, if appropriate, mitigate project impacts upon historic, archaeological, and paleontological resources with the project boundary.

**Goal 8:** *Develop and maintain recreational facilities as economically feasible, and that meet the needs of the community for recreational activities, relaxation, and social interaction.*

**Consistency:** The project will include a 10-acre park and a 50-acre open space area for hiking and other passive recreational uses. These facilities will be utilized by employees of the West Campus Upper Plateau, surrounding residents, and visitors.

**Goal 9:** *Create a network of open space areas and linkages throughout the Planning Area that serves to preserve natural resources, protect health and safety, contributes to the character of the community, provide active and passive recreational use, as well as visual and physical relief from urban development.*

**Consistency:** The West Campus Upper Plateau Specific Plan provides for approximately 445 acres of land dedicated for park, open space, and conservation use. This will not only help provide transitional buffering between existing residential and proposed industrial, business park and mixed-use activities, but also allow for passive recreational use and habitat.

**Goal 10:** *Establish standards for scenic corridors, trails and vistas that contributes to the quality of the planning area.*

**Consistency:** The Specific Plan project site will provide a 10-acre park and 50-acre open space area for passive use via a trail system. Additionally, a multi-purpose trail is proposed along the western side of Barton Street that will serve as access to the park and open space areas in addition to through trips. The project site design will provide opportunity to take advantage of territorial and scenic views afforded from this location.

## 6. Safety/Risk Management

**Goal 1:** *Minimize injury and loss of life, property damage, and other impacts caused by seismic shaking, fault rupture, ground failure, and landslides.*

**Consistency:** A geological reconnaissance has been conducted for the property. That study revealed that there are no active or inactive faults crossing the property and that the property is suitable for development.

**Goal 2:** *Minimize grading and otherwise changing the natural topography, while protecting the public safety and property from geologic hazards.*

**Consistency:** Grading within the Specific Plan area is designed to minimize impacts to the existing topography. The project will incorporate grading development standards and recommendations, which will minimize any potential geotechnical and site development constraints that occur on-site.

**Goal 3:** *Minimize injury, loss of life, property damage, and economic and social disruption caused by flood hazards.*

**Consistency:** The West Campus Upper Plateau will provide a number of drainage facilities to ensure flood hazards associated with the project site are managed in accordance with applicable, state, regional and local requirements.

**Goal 4:** *Reduce threats to public safety and protect property from wildland and urban fire hazards.*

**Consistency:** The West Campus Upper Plateau shall comply with applicable regulations and guidelines relating to brush management and fire protections services.

**Goal 5:** *Reduce the potential for hazardous material exposure or contamination in the Planning Area.*

**Consistency:** The West Campus Upper Plateau shall comply with regulations and guidelines relating to hazardous material exposure/contamination.

**Goal 6:** *Ensure to the fullest extent practical that, in the event of a major disaster, critical structures and facilities remain safe and functional.*

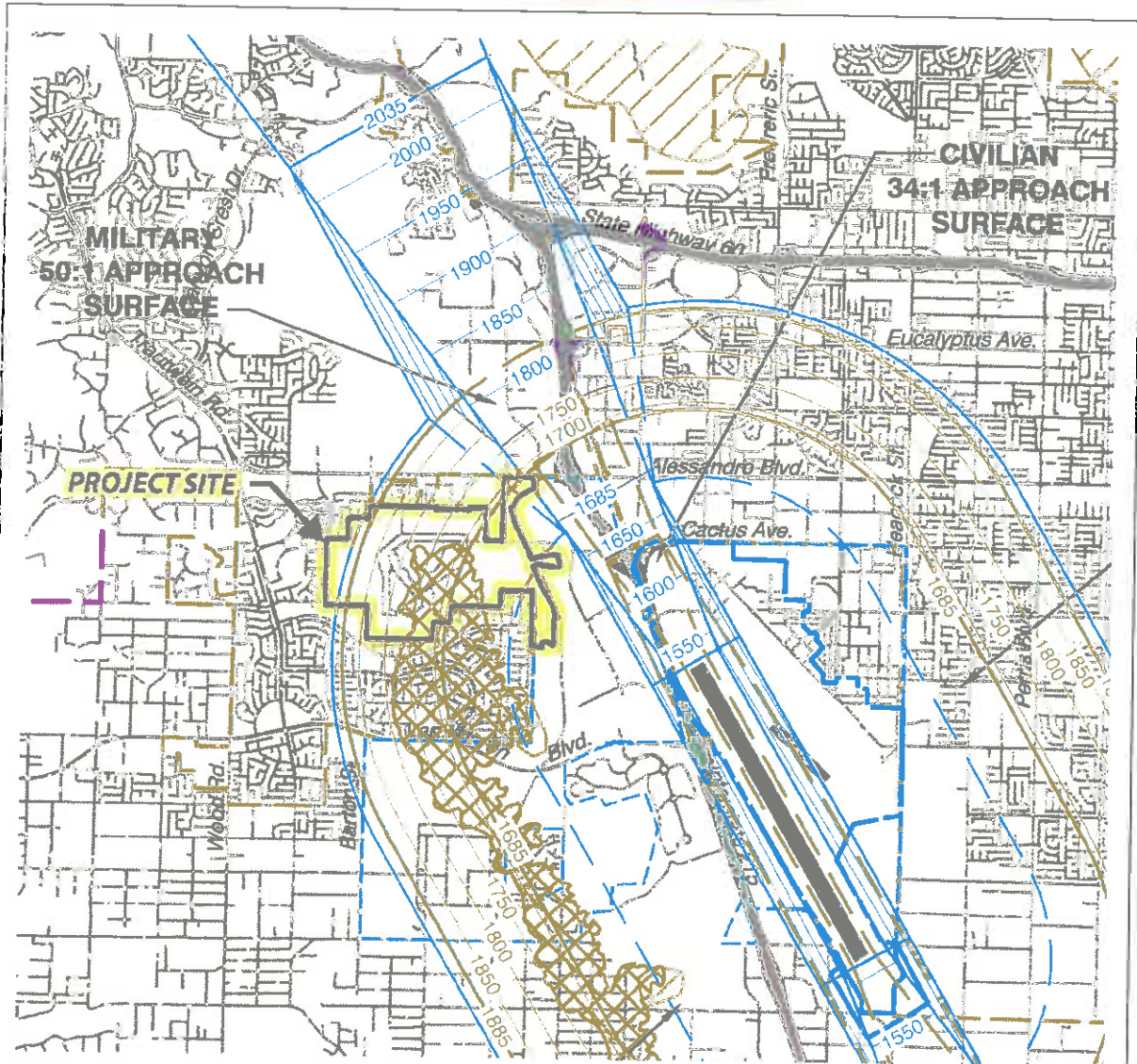
**Consistency:** The West Campus Upper Plateau shall comply with regulations and guidelines relating to the functionality of critical structures in the event of a major disaster.

**Goal 7:** *Reduce the possible risk of upset, injury and loss of life, property damage, and other impacts associated with an aviation facility.*

**Consistency:** The West Campus Upper Plateau is designed to incorporate appropriate uses within the development-limited areas as defined in the Air Installation Compatible Use Zone (AICUZ) Study done in 1998. The project will also comply with the Airport Land Use Plan.

**Goal 8:** *Plan for emergency response and recovery from natural and urban disasters.*

**Consistency:** The West Campus Upper Plateau shall comply with appropriate and applicable regulations and guidelines relating to emergency response and recovery from natural and urban disasters.



**LEGEND**

FAR Part 77

- Military Surfaces
- Civilian Surfaces

} Dashed line indicates other set of surfaces is controlling

Terrain Penetration of FAR Part 77 Surfaces

- Military
- Civilian

Boundary Lines

- March Air Reserve Base / Inland Port Airport
- March Joint Powers Authority Property Line
- City Limits

Source(s): March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan (11-13-2014)



Not to Scale



MARB Airspace Protection Surfaces

Specific Plan No. XXX

Appendix A

<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>
<u>TREES</u>	
<i>Arbutus unedo</i>	Strawberry Tree
<i>Arbutus 'Marina'</i>	Marina Strawberry Tree
<i>Caesalpinia cocalaco</i>	Cascalote
<i>Callistemon viminalis</i>	Weeping Bottlebrush
<i>Cercidium 'Desert Museum'</i>	Desert Museum Palo Verde
<i>Cercidium floridum</i>	Blue Palo Verde
<i>Cercis occidentalis</i>	Western Redbud
<i>Cercis canadensis 'Hearts of Gold'</i>	Hearts of Gold Redbud
<i>Chilopsis linearis 'Burgundy Lace'</i>	Burgundy Lace Desert Willow
<i>Chitalpa tashkentensis</i>	Chitalpa
<i>Cinnamomum camphora</i>	Camphor Tree
<i>Eriobotrya japonica</i>	Loquat
<i>Eriobotrya deflexa</i>	Bronze Loquat
<i>Erythrina caffra</i>	Kaffirboom Coral Tree
<i>Fraxinus oxycarpa 'Raywood'</i>	Raywood Ash
<i>Geijera parviflora</i>	Australian Willow
<i>Ginkgo biloba</i>	Maidenhair Tree
<i>Juglans californica</i>	California Walnut
<i>Juglans hindsii</i>	California Black Walnut
<i>Koelreuteria paniculata</i>	Goldenrain Tree
<i>Lagerstroemia indica</i>	Crape Myrtle
<i>Laurus nobilis 'Saratoga'</i>	Saratoga Laurel
<i>Lophostemon confertus</i>	Brisbane Box
<i>Magnolia grandiflora</i>	Southern Magnolia
<i>Olea europaea 'Wilsonii', 'Swan Hill'</i>	Fruitless Olive Tree
<i>Pinus edulis</i>	Colorado Pinyon
<i>Pinus halepensis</i>	Aleppo Pine
<i>Pistacia chinensis</i>	Chinese Pistache
<i>Platanus x acerifolia</i>	London Plane Tree
<i>Platanus racemosa 'Bloodgood'</i>	Bloodgood London Plane Tree
<i>Prosopis chilensis</i>	Thornless Chilean Mesquite
<i>Pyrus calleryana 'Aristocrat'</i>	Aristocrat Pear
<i>Stenocarpus sinuatus</i>	Firewheel Tree
<i>Tabebuia impetiginosa</i>	Pink Trumpet Tree
<i>Tecoma stans</i>	Yellow Bells
<i>Quercus spp.</i>	Oak
<i>Ulmus parvifolia 'True Green'</i>	True Green Evergreen Elm

BOTANICAL NAMECOMMON NAMESHRUBS

<i>Achillea filipendulina</i>	Fern Leaf Yarrow
<i>Achillea millefolium</i>	Yarrow
<i>Agave</i> spp.	Agave
<i>Aloe</i> spp.	Aloe
<i>Anigozanthos</i>	Kangaroo Paw
<i>Anisacanthus quadrafidus</i> var. <i>Wrightii</i>	Flame Acanthus
<i>Aristida purpurea</i>	Purple Three Awn
<i>Bouteloua gracilis</i>	Blue Grama Grass
<i>Bulbine frutescens</i>	Stalked Bulbine
<i>Calliandra californica</i>	Baja Fairy Duster
<i>Calliandra eriophylla</i>	Pink Fairy Duster
<i>Callistemon viminalis</i> 'Little John'	Little John Callistemon
<i>Carex tumulicola</i>	Berkeley Sedge
<i>Ceanothus</i> spp.	California Lilac
<i>Cistus</i> x <i>pulverulentus</i> 'Sunset'	Sunset Rockrose
<i>Cistus</i> x <i>salviifolius</i>	Sageleaf Rockrose
<i>Correa pulchella</i>	Pink Australian Fuchsia
<i>Dalea capitata</i>	Lemon Dalea
<i>Dianella revoluta</i> 'Little Rev'	Little Rev Flax Lily
<i>Dianella tasmanica</i>	Variegated Flax Lily
<i>Dietes bicolor</i>	Fortnight Lily
<i>Dodonaea viscosa</i> 'Purpurea'	Purple Hopseed
<i>Dudleya pulverulenta</i>	Chalk Liveforever
<i>Elaeagnus pungens</i>	Silverthorn
<i>Epilobium canum</i>	California Fuchsia
<i>Eriophyllum confertiflorum</i>	Golden Yarrow
<i>Euphorbia rigida</i>	Silver Spurge
<i>Festuca mairei</i>	Atlas Fescue
<i>Hesperaloe parviflora</i>	Texas Yucca
<i>Junus patens</i>	Gray Rush
<i>Justicia californica</i>	Chuparosa
<i>Kniphofia uvaria</i>	Torch Lily
<i>Lantana</i> spp.	Lantana
<i>Lavandula</i> spp.	Lavender
<i>Leucophyllum frutescens</i> + cvs	Texas Ranger
<i>Leymus condensatus</i> 'Canyon Prince'	Canyon Prince Wild Rye
<i>Lupinus albifrons</i>	Silver Bush Lupine
<i>Nolina parryi</i>	Parry's Bear Grass

BOTANICAL NAMECOMMON NAMESHRUBS

Olea europea 'Little Ollie'	Dwarf Olive
Penstemon spp.	Penstemon
Phlomis fruticosa	Jerusalem Sage
Phormium spp.	New Zealand Flax
Pittosporum spp.	Pittosporum
Rhaphiolepis spp.	Indian Hawthorn
Romneya coulteri	Matilija Poppy
Rosemarinus spp.	Rosemary
Santolina spp.	Lavender Cotton
Senna artimisioides	Feathery Cassia
Stachys byzantina	Lamb's Ear
Trichostema lanatum	Woolly Blue Curls
Westringia fruticosa	Coast Rosemary
Yucca filamentosa	Yucca

GROUNDCOVERS

Acacia redolens 'Low Boy'	Prostrate Acacia
Carex flacca	Blue Sedge
Carex praegracilis	Clustered Field Sedge
Dalea greggii	Trailing Indigo Bush
Dymondia margaretae	Silver Carpet
Elymus triticoides	Creeping Wild Rye
Lantana montevidensis	Trailing Lantana
Lantana x 'New Gold'	New Gold Lantana
Myoporum parvifolium + cvs	Creeping Myoporum
Rosmarinus officinalis 'Prostrate'	Creeping Rosemary
Senecio spp.	Blue Chalksticks

*\* In an effort to reduce wildlife hazards to aircraft operations, plant palette priority shall be given to plants listed in the Riverside County Airport Land Use Commission's "Landscaping Near Airports" brochure. See Specific Plan Section 4.5.1 - Plant Palette for additional information.*

**NOTICE OF PUBLIC HEARING**  
**RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**  
[www.rcaluc.org](http://www.rcaluc.org)

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. **Information on how to participate in the hearing will be available on the ALUC website at [www.rcaluc.org](http://www.rcaluc.org).** The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893.**

The March Joint Powers Authority Planning Department should be contacted on non-ALUC issues. For more information, please contact March Joint Powers Authority Planner Mr. Dan Fairbanks at 951-656-7000.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website [www.rcaluc.org](http://www.rcaluc.org). Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to [prull@rivco.org](mailto:prull@rivco.org). Individuals with disabilities requiring reasonable modifications or accommodations, please contact Barbara Santos at (951) 955-5132.

**PLACE OF HEARING:** Riverside County Administration Center  
4080 Lemon Street, 1<sup>st</sup> Floor Board Chambers  
Riverside California

**DATE OF HEARING:** May 12, 2022

**TIME OF HEARING:** 9:30 A.M.

**CASE DESCRIPTION:**

ZAP1515MA22 – Meridian Park West, LLC (Representative: Waypoint Property Group) – March Joint Powers Authority Case Nos. GP 21-01 (General Plan Amendment), SP21-01 (Specific Plan), PP21-03 and PP21-04 (Plot Plans), TTM38063 (Tentative Tract Map). The applicant proposes the West Campus Upper Plateau Specific Plan, encompassing 817.9 acres within multiple Airport Compatibility Zones located southerly of Alessandro Boulevard, westerly of Meridian Parkway, northerly of Grover Community Drive, and easterly of Trautwein Road. As part of this project, the applicant proposes amending the General Plan land uses to increase Parks/Recreation and Open Space from 122 acres to 453 acres, eliminate approximately 622.5 acres of Business Park, eliminate approximately 63 acres of Industrial property, approve a 2.6 acre Public Facility area for an existing water tank, and adopt the West Campus Upper Plateau Specific Plan (SP-) on approximately 351 acres and creating policies for the future recordation of a 445 acre Conservation Area. The applicant also proposes to adopt Specific Plan SP-9 containing development standards, design guidelines, infrastructure master plans, maintenance responsibilities, phasing schedule, and implementation procedures necessary to develop a 359 acre business park and adjacent park space. The Specific Plan proposes 43.1 acres of Mixed Use, 66.4 acres of Business Park, 143.3



acres of Industrial, 28.9 acres of streets and roadways, 10 acres of developed Parks/Recreation/Open Space, 64.5 acres of undeveloped Parks/Recreation/Open Space, and 3.5 acres of Public Facilities. The Specific Plan will adopt zoning on the properties consistent with the Specific Plan land use designations. The applicant also proposes to construct 2 industrial buildings with mezzanines on separate parcels totaling 1,820,000 square feet on (combined) 115.88 acres, located northerly of (future roads) Bunker Hill Drive, easterly of Airman Drive, southerly of Arclight Drive, and westerly of Linebacker Drive. (Only development entitlements for PP21-03 and PP21-04 have been submitted with this application. No development projects for the other parcels have been proposed at this time.) The applicant also proposes a tentative tract map to divide 359.6 acres into 17 buildable lots and 7 lettered lots for streets/open space. (Airport Compatibility Zones B1, B2, C1, and C2 High Terrain Zone of the March Air Reserve Base/Inland Port Airport Influence Area).



# RIVERSIDE COUNTY

## AIRPORT LAND USE COMMISSION

March Zone C1+C2

### APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP1515MA22 DATE SUBMITTED: 3/17/22

#### APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

Applicant	<u>Meridian Park West, LLC</u>	Phone Number	<u>(949) 200-6755</u>
Mailing Address	<u>567 San Nicolas Dr., Suite 270</u>	Email	<u>kgordon@waypointpg.com</u>
	<u>Newport Beach, CA 92660</u>		

Representative	<u>Jeff Gordon</u>	Phone Number	<u>(949) 279-7339</u>
Mailing Address	<u>567 San Nicolas Dr., Suite 270</u>	Email	<u>kgordon@waypointpg.com</u>
	<u>Newport Beach, CA 92660</u>		

Property Owner	<u>March Joint Powers Authority</u>	Phone Number	<u>(951) 656-7000</u>
Mailing Address	<u>14205 Meridian Parkway, Suite 140</u>	Email	<u>fairbanks@marchjpa.com</u>
	<u>Riverside, CA 92518</u>		

#### LOCAL JURISDICTION AGENCY

Local Agency Name	<u>March Joint Powers Authority</u>	Phone Number	<u>(951) 656-7000</u>
Staff Contact	<u>Dan Fairbanks</u>	Email	<u>fairbanks@marchjpa.com</u>
Mailing Address	<u>14205 Meridian Parkway, Suite 140</u>	Case Type	
	<u>Riverside, CA 92518</u>		

Local Agency Project No	<u>Specific Plan = SP 21-01, VTPM = 38063, PP 21-03, PP 21-04</u>	<input checked="" type="checkbox"/> General Plan / Specific Plan Amendment
	<u>GP-21-01</u>	<input type="checkbox"/> Zoning Ordinance Amendment
		<input type="checkbox"/> Subdivision Parcel Map / Tentative Tract
		<input type="checkbox"/> Use Permit
		<input checked="" type="checkbox"/> Site Plan Review/Plot Plan
		<input type="checkbox"/> Other

#### PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address vacant land generally south of Alessandro Blvd, west of Meridian Parkway, north of Orangecrest and east of Barton Road in the county of Riverside.

Assessor's Parcel No. 276-170-07, 297-080-002/3, 297-090-001/-002/-008/-009

Subdivision Name Meridian West Upper Plateau

Lot Number N/A

Gross Parcel Size	<u>360-acres</u>
Nearest Airport and distance from Airport	<u>March approx 1 mile</u>

#### PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe) Vacant land. Formerly weapons storage area for March Air Force Base.

Proposed Land Use (describe)	Industrial Park with various commercial land uses.: Business park, Mixed-Use and Industrial	
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)	N/A
For Other Land Uses (See Appendix C)	Hours of Operation	
	Number of People on Site	Maximum Number
	Method of Calculation	
Height Data	Site Elevation (above mean sea level)	1,732'
	Height of buildings or structures (from the ground)	50'
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?	
	<input type="checkbox"/> Yes <input type="checkbox"/> No	
	If yes, describe	<p>No. All lighting will be oriented downwards and maintain low glare levels.</p> <p>Developer has built multiple buildings in the March influenced areas and is aware of the lighting protocols in the area.</p>

A. **NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. **REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.

C. **SUBMISSION PACKAGE:**

1. . . . . Completed ALUC Application Form
1. . . . . ALUC fee payment
1. . . . . Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
1. . . . . Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
1. . . . . CD with digital files of the plans (pdf)
1. . . . . Vicinity Map (8.5x11)
1. . . . . Detailed project description
1. . . . . Local jurisdiction project transmittal
3. . . . . Gummed address labels for applicant/representative/property owner/local jurisdiction planner
3. . . . . Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. (Only required if the project is scheduled for a public hearing Commission meeting)

# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

## STAFF REPORT

### ADMINISTRATIVE ITEMS

#### 5.1 Director's Approvals

- A. During the period of March 16, 2022, through April 15, 2022, as authorized pursuant to Section 1.5.2(d) of the 2004 Riverside County Airport Land Use Compatibility Plan, ALUC Director Paul Rull reviewed two non-legislative cases within the Banning and Riverside Municipal Airport Influence Areas.

ZAP1046BA22 (Zone D Banning AIA) pertains to City of Banning Case No. CUP20-8012 (Conditional Use Permit), a proposal to establish a truck repair and truck terminal parking facility on 4.57 acres, located southerly of 8<sup>th</sup> street and westerly of Barbour Avenue. The site is located within Airport Compatibility Zone D of the Banning Municipal Airport Influence Area (AIA). Zone D restricts non-residential intensity to 200 people per average acre and 800 people per single acre. The project proposes 2,400 square feet of office area, 6,000 square feet of repair shop area, and 6,580 square feet of truck storage area, accommodating 64 people, resulting in an average acre intensity of 14 people and a single acre intensity of 54 people, both of which are consistent with the Zone D intensity criteria. The elevation of Runway 8-26 at its westerly terminus is 2,119 feet above mean sea level (AMSL). At a distance of approximately 7,345 feet from the runway to the site, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with top of roof exceeding 2,192 feet AMSL. The site's finished floor elevation is 2,320 feet AMSL and the proposed building height is 28 feet, for a top point elevation of 2,348 feet AMSL. Therefore, FAA Obstruction Evaluation Service review for height/elevation reasons was required. The applicant submitted Form 7460-1 to the FAA OES. A "Determination of No Hazard to Air Navigation" letter for Aeronautical Study No. 2022-AWP-1757-OE was issued on March 14, 2022. The study revealed that the proposed facility would not exceed obstruction standards and would not be a hazard to air navigation provided conditions are met. These FAA OES conditions have been incorporated into this finding. Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33B). The project is located 7,345 feet from the runway, and therefore would be subject to the above requirement. The project would utilize bioretention basins, which are not recommended in the vicinity of airports due to the potential that such areas could provide food, water, and shelter for hazardous wildlife. Pursuant to the study "Wildlife Hazard Management at Riverside County Airports: Background and Policy", October 2018, by Mead & Hunt, which is the basis of the brochure titled "Airports, Wildlife and Stormwater Management", such basins are to be avoided in Zones D and E, unless they provide for a 48-hour drawdown and propose landscaping that is not attractive to hazardous wildlife. The project has been conditioned to be consistent with these standards in order to reduce bird attractant: 1) new basins are to be designed so as to provide for a maximum 48-hour detention period following the conclusion of a storm event, and to remain totally dry between rainfalls, and 2) any landscaping proposed in the detention basin shall be in accordance with the ALUC "Landscaping Near Airports" and "Airports, Wildlife and Stormwater Management" brochures.

ALUC Director Paul Rull issued a determination of consistency for this project on March 16, 2022.

\*\*\*\*\*

ZAP1103RI22 (Zone E Riverside Municipal AIA) pertains to City of Jurupa Valley Case No. MA21294 (Plot Plan), a proposal to construct a 7, 380 square foot Auto Zone commercial retail building on 1.37 acres, located on the southwest corner of Limonite Avenue and Corey Street. The site is located within Airport Compatibility Zone E of the Riverside Municipal Airport Influence Area (AIA). Within Compatibility Zone E of the Riverside Municipal Airport Land Use Compatibility Plan, non-residential intensity is not restricted. The elevation of Runway 9-27 at its westerly terminus is 757.6 feet above mean sea level (AMSL). At a distance of approximately 12,733 feet from the runway to the site, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 884 feet AMSL. The project site elevation is 693 feet AMSL. With a maximum building height of 27 feet, the resulting top point elevation is 720 feet AMSL. Therefore, review of buildings by the FAA Obstruction Evaluation Service (FAAOES) for height/elevation reasons was not required.

ALUC Director Paul Rull issued a determination of consistency for this project on April 14, 2022.

**5.2** Update March Air Reserve Base Compatibility Use Study (CUS)  
Presentation by Project Director Simon Housman or his designee.

**5.3** Digitizing the ALUC Application Process  
Presentation by ALUC Director Paul Rull or his designee.

X:\ALUC Administrative Items\Admin. 2022\Admin Item 5-12-22.doc



# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

March 16, 2022

Mark De Manincor, Project Planner  
City of Banning Community Development Department – Planning Division  
99 E. Ramsey Street  
Banning, CA 92220

**CHAIR**  
Steven Stewart  
Palm Springs

**VICE CHAIR**  
Steve Manos  
Lake Esirnore

**COMMISSIONERS**

Arthur Butler  
Riverside

John Lyon  
Riverside

Russell Betts  
Desert Hot Springs

Richard Stewart  
Moreno Valley

Michael Geller  
Riverside

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –  
DIRECTOR’S DETERMINATION**

File No.: ZAP1046BA22  
Related File No.: CUP21-8003 (Conditional Use Permit)  
APN: 540-250-002

Dear Mr. DeManincor,

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to ALUC’s general delegation as per Policy 1.5.2(a) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed City of Banning Case No. CUP20-8012 (Conditional Use Permit), a proposal to establish a truck repair and truck terminal parkin facility on 4.57 acres, located southerly of 8<sup>th</sup> street and westerly of Barbour Avenue.

The site is located within Airport Compatibility Zone D of the Banning Municipal Airport Influence Area (AIA). Zone D restricts non-residential intensity to 200 people per average acre and 800 people per single acre. The project proposes 2,400 square feet of office area, 6,000 square feet of repair shop area, and 6,580 square feet of truck storage area, accommodating 64 people, resulting in an average acre intensity of 14 people and a single acre intensity of 54 people, both of which are consistent with the Zone D intensity criteria.

The elevation of Runway 8-26 at its westerly terminus is 2,119 feet above mean sea level (AMSL). At a distance of approximately 7,345 feet from the runway to the site, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with top of roof exceeding 2,192 feet AMSL. The site’s finished floor elevation is 2,320 feet AMSL and the proposed building height is 28 feet, for a top point elevation of 2,348 feet AMSL. Therefore, FAA Obstruction Evaluation Service review for height/elevation reasons was required. The applicant submitted Form 7460-1 to the FAA OES. A “Determination of No Hazard to Air Navigation” letter for Aeronautical Study No. 2022-AWP-1757-OE was issued on March 14, 2022. The study revealed that the proposed facility would not exceed obstruction standards and would not be a hazard to air navigation provided conditions are met. These FAA OES conditions have been incorporated into this finding.

Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33B). The project is located 7,345 feet from the runway, and therefore would be subject to the above requirement. The project

Simon A. Housman  
Barbara Santos  
Jackie Vega

County Administrative Center  
4080 Lamon St., 14<sup>th</sup> Floor  
Riverside, CA 92501  
(951) 955-6132

[www.rcaluc.org](http://www.rcaluc.org)

**STAFF**

Director  
Paul Rull

would utilize bioretention basins, which are not recommended in the vicinity of airports due to the potential that such areas could provide food, water, and shelter for hazardous wildlife. Pursuant to the study "Wildlife Hazard Management at Riverside County Airports: Background and Policy", October 2018, by Mead & Hunt, which is the basis of the brochure titled "Airports, Wildlife and Stormwater Management", such basins are to be avoided in Zones D and E, unless they provide for a 48-hour drawdown and propose landscaping that is not attractive to hazardous wildlife. The project has been conditioned to be consistent with these standards in order to reduce bird attractant: 1) new basins are to be designed so as to provide for a maximum 48-hour detention period following the conclusion of a storm event, and to remain totally dry between rainfalls, and 2) any landscaping proposed in the detention basin shall be in accordance with the ALUC "Landscaping Near Airports" and "Airports, Wildlife and Stormwater Management" brochures.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2004 Banning Municipal Airport Land Use Compatibility Plan, as amended in 2016, provided that the City of Banning applies the following recommended conditions:

**CONDITIONS:**

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, outdoor production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Highly noise-sensitive outdoor nonresidential uses.
  - (f) Any use which results in a hazard to flight, including physical (e.g., tall objects), visual, and electronic forms of interference with the safety of aircraft operations.
3. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property and be recorded as a deed notice. In the event that the Office of the Riverside County Assessor-Clerk-Recorder declines to record said notice, the text of the notice shall be included on the Environmental Constraint Sheet (ECS) of the final parcel map, if an ECS is otherwise required.

4. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the stormwater basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at [RCALUC.ORG](http://RCALUC.ORG) which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin

5. This project has been evaluated as a truck repair and truck terminal parking facility with a 2,400 square feet of office area, 6,000 square feet of repair shop area, and 6,580 square feet of truck storage area. Any increase in building area or change in use will require an amended review by the Airport Land Use Commission.
6. The Federal Aviation Administration has conducted aeronautical studies of the proposed structure (Aeronautical Study No. 2022-AWP-1757-OE) and has determined that neither marking nor lighting of the structures is necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 L Change 2 and shall be maintained in accordance therewith for the life of the project.
7. The maximum height of the proposed structures to top point shall not exceed 28 feet above ground level, and the maximum elevation at the top of the structures shall not exceed 2,320 feet above mean sea level.
8. The maximum height and top point elevation specified above shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in structure height or elevation shall not require further review by the Airport Land Use Commission.
9. Temporary construction equipment used during actual construction of the structures shall not exceed 28 feet in height and a maximum elevation of 2,320 feet above mean sea level, unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.
10. Within five (5) days after construction of each structure reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned or a decision is made not to construct the applicable structure.



If you have any questions, please contact me at (951) 955-6893.

Sincerely,  
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



---

Paul Rull, ALUC Director

Attachment: Notice of Airport in Vicinity  
Aeronautical Study No. 2022-AWP-1757-OE

cc: Zamora Lease and Rental, LLC (applicant)  
The Planning Consortium, W. Dean Brown (representative)  
Hector Cardenas (property owner)  
Art Vela, P.E., City of Banning Director of Public Works  
Carl Szoyka, Manager, Banning Municipal Airport  
ALUC Case File

X:\AIRPORT CASE FILES\Banning\ZAP1046BA22\ZAP1046BA22.LTR.doc

# **NOTICE OF AIRPORT IN VICINITY**

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

# NOTICE

**THERE IS AN AIRPORT NEARBY.  
THIS STORM WATER BASIN IS DESIGNED TO HOLD  
STORM WATER FOR ONLY 48 HOURS AND  
NOT TO ATTRACT BIRDS**

**PROPER MAINTENANCE IS NECESSARY TO AVOID  
BIRD STRIKES**

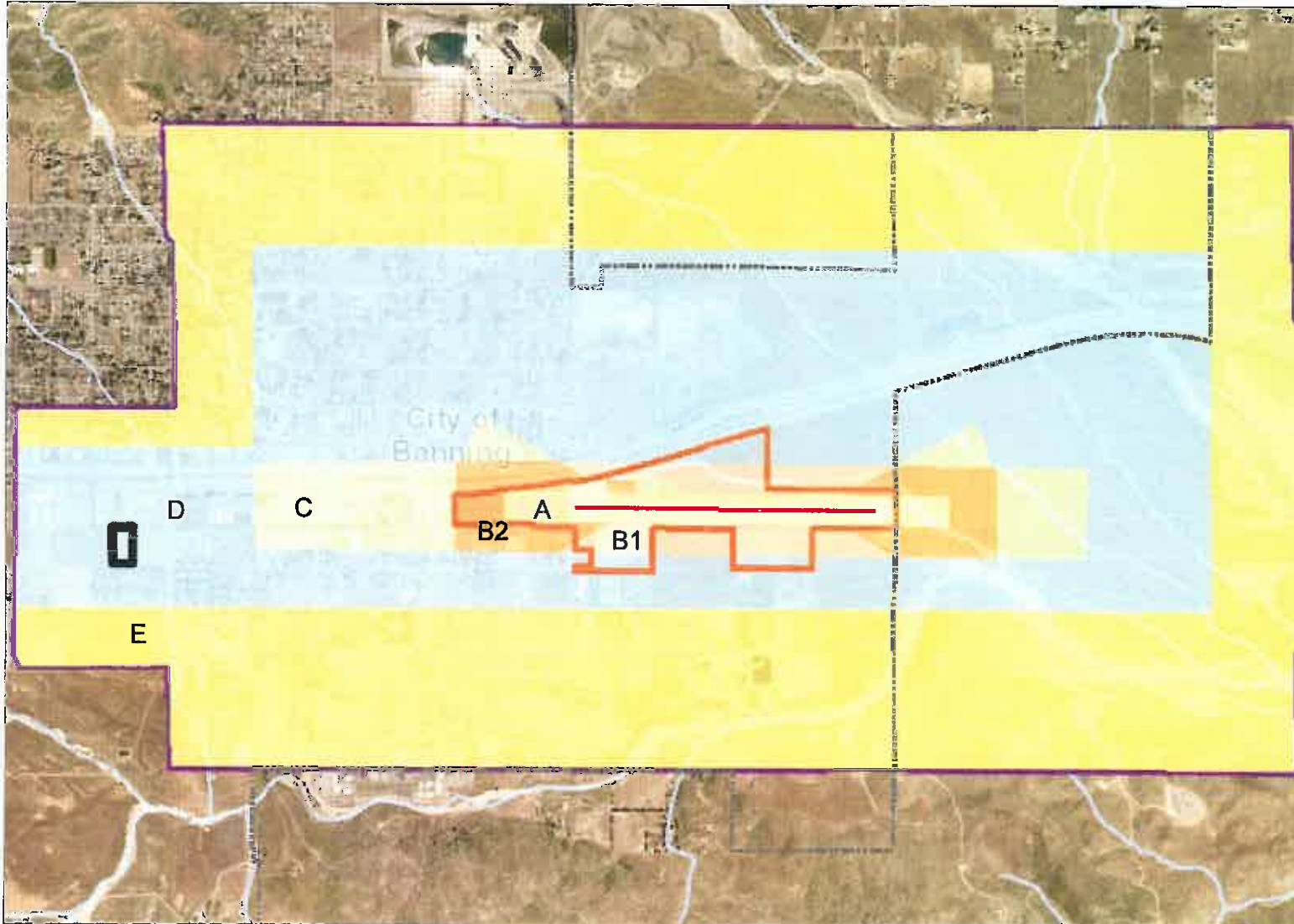


**IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:**

**Name:** \_\_\_\_\_

**Phone:** \_\_\_\_\_

# Map My County Map



- Legend**
- Runways
  - Airports
  - Airport Influence Areas
  - Airport Compatibility Zones**
  - OTHER COMPATIBILITY ZONE
  - A
  - A-EXC1
  - B1
  - B1-APZ I
  - B1-APZ I-EXC1
  - B1-APZ II
  - B1-APZ II-EXC1
  - B1-EXC1
  - B2
  - B2-EXC1
  - C
  - C1
  - C1-EXC1
  - C1-EXC3
  - C1-EXC4
  - C1-HIGHT
  - C2
  - C2-EXC1
  - C2-EXC2
  - C2-EXC3
  - C2-EXC5
  - C2-EXC6



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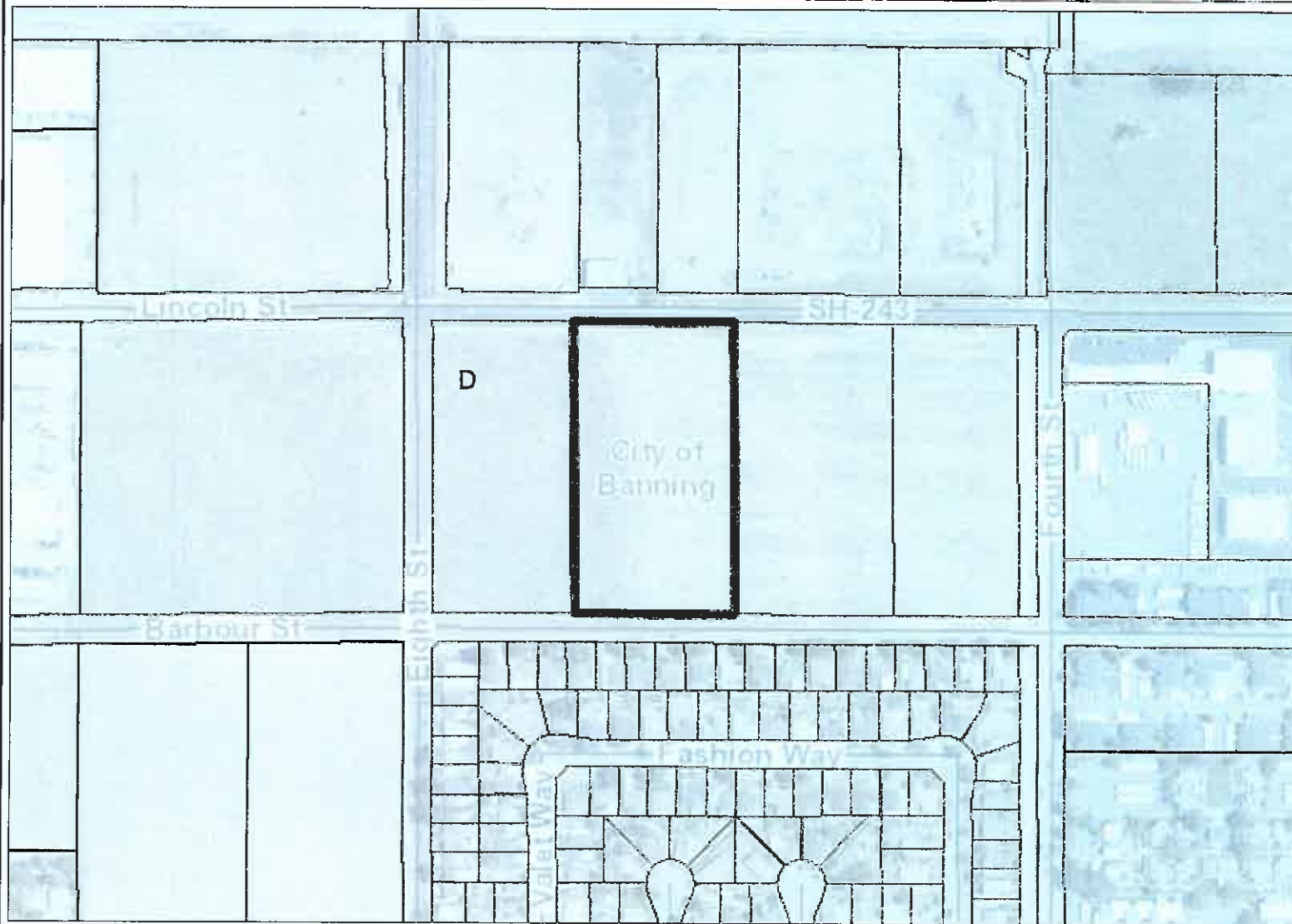
**Notes**



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# Map My County Map



**Legend**

- Parcels
- Runways
- Airports
- Airport Influence Areas

**Airport Compatibility Zones**

- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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**Notes**



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# Map My County Map



## Legend

-  Parcels
-  County Centerline Names
-  County Centerlines
-  Blueline Streams
-  City Areas
-  World Street Map



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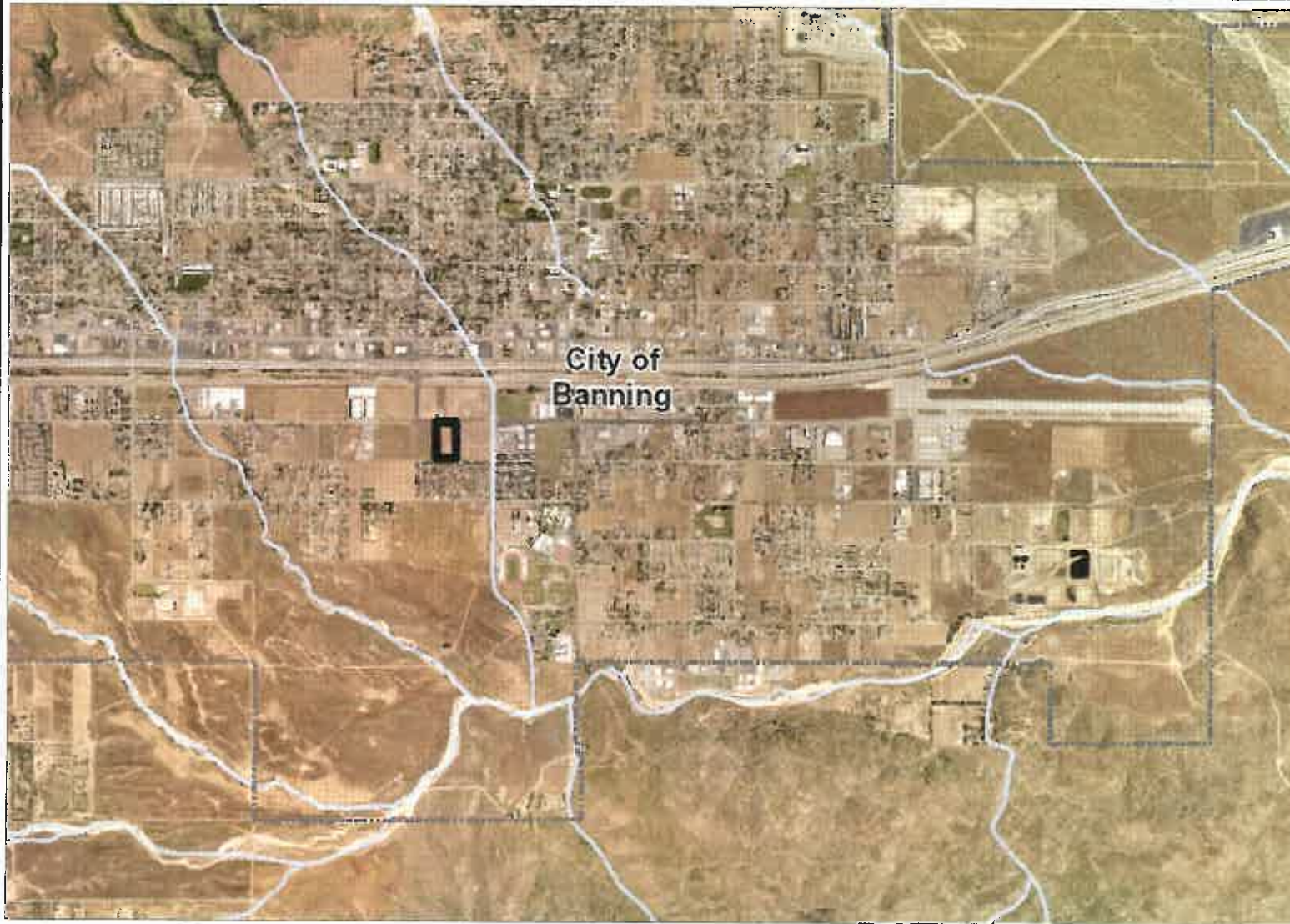
## Notes

0 385 770 Feet

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# Map My County Map



## Legend

-  Blueline Streams
-  City Areas
-  World Street Map



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## Notes



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# Map My County Map



- Legend**
- Blue line Streams
  - Dashed line City Areas
  - World Street Map



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**Notes**

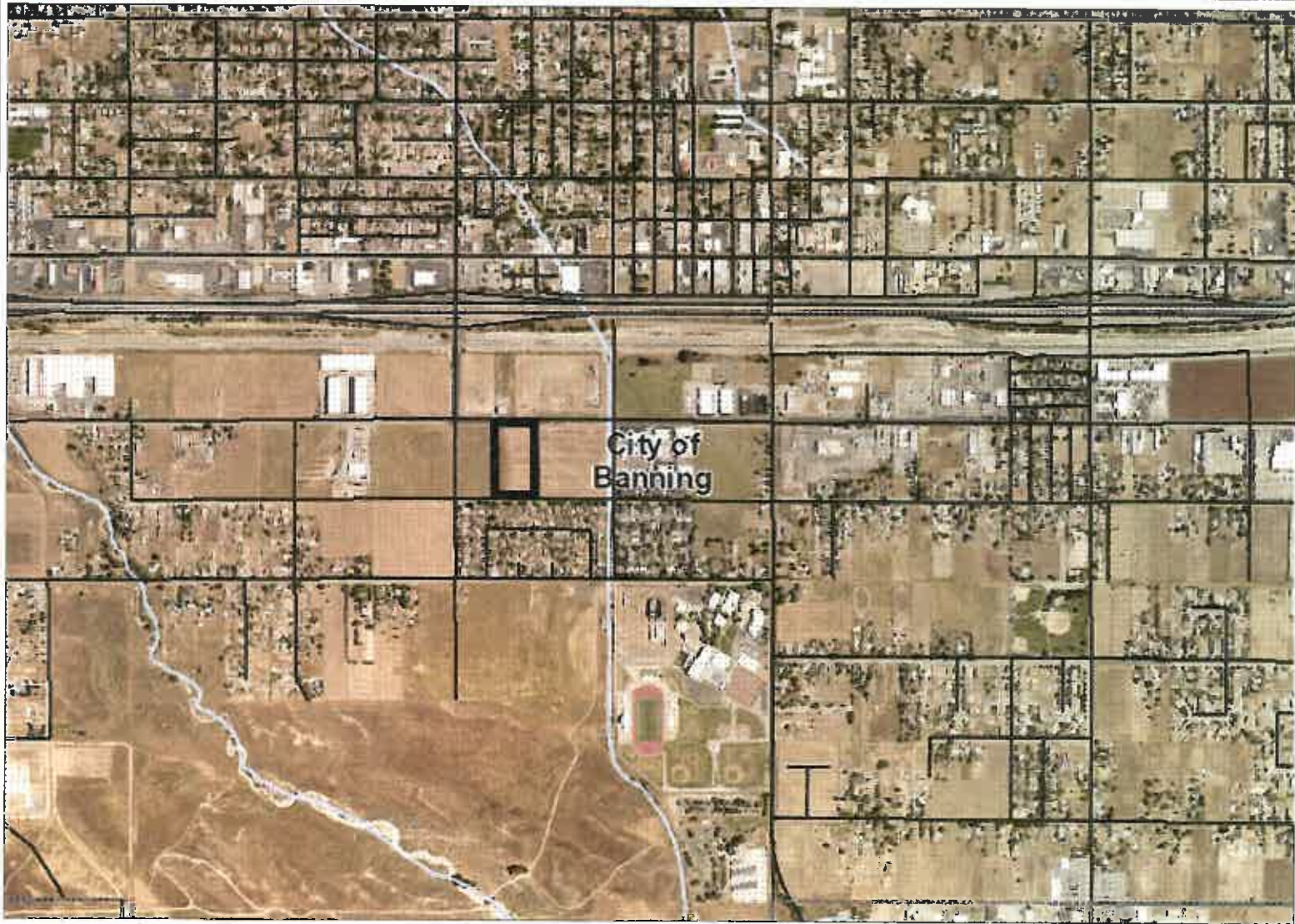


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# Map My County Map



- Legend**
- County Centerline Names
  - County Centerlines
  - Blueline Streams
  - City Areas
  - World Street Map



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## Notes



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**GENERAL INFORMATION:**  
**OWNER:**  
 ZAKORA LEASE & RENTAL LLC,  
 10861 LARCH AVENUE  
 BLOOMINGTON, CA 92318

**ENGINEER:**  
 GK CONSULTING, INC.  
 PO BOX 1041  
 UPLAND, CA 91784  
 TEL: 909-870-8187

**PROJECT MANAGER:**  
 AMR. CASTILLO  
 TEL: 909-867-2981

**PROPERTY INFORMATION:**  
**SITE ADDRESS:**  
 300 FT EAST OF WEST LINCOLN ST. & WEST  
 6TH ST,  
 BANNING, CA 92220  
 APN: 640-250-002  
 LOT SIZE: 4.08 ACRES

**CURRENT AND PROPOSED LAND USE:**  
 INDUSTRIAL

**BUSINESS OPERATION:**  
 TRUCK REPAIR SHOP AND  
 TRUCK PARKING TERMINAL

**LEGAL DESCRIPTION:**  
 4.87 ACRES IN PDR BLK 291 MB PM 80 SUB  
 OF LANDS ADJOINING BANNING ON SW  
 BLOCK 291 OF SUBDIVISION OF LANDS  
 ADJOINING BANNING ON THE SOUTH-WEST,  
 AS SHOWN BY MAP ON FILE IN BOOK 14,  
 PAGE 882 OF MAPS, RECORDS OF SAN  
 DIEGO COUNTY, CALIFORNIA.

**TAX ASSESSOR'S PARCEL NO.**  
 640-250-002

**PROPOSED SCOPE OF WORK:**  
 PROPOSED A NEW OFFICEWAREHOUSE  
 BUILDING  
 - 2,400 SF OFFICE AREA  
 - 8,000 SF REPAIR SHOP  
 - 8,800 SF TRUCK STORAGE AREA  
 BUILDING HEIGHT - 27'-6"

**SETBACKS:**  
 FRONT: 40 FT  
 SIDER: 10 FT  
 REAR: 20 FT

**DRAWING SHEET SCHEDULE**  
 A0 - SITE PLAN / PROJECT DATA  
 A1 - BUILDING FLOOR / ROOF PLAN  
 A2 - BUILDING ELEVATIONS  
 A3 - BUILDING COLOR ELEVATIONS  
 A4 - BUILDING 3D RENDERING  
 LT-1 SITE LIGHTING PLAN  
 LT-2 LIGHTING FIXTURE SPECIFICATION  
 1 OF 1 - PRELIMINARY GRADING PLAN  
 1 OF 1 - CONCEPTUAL LANDSCAPING PLAN



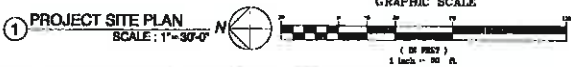
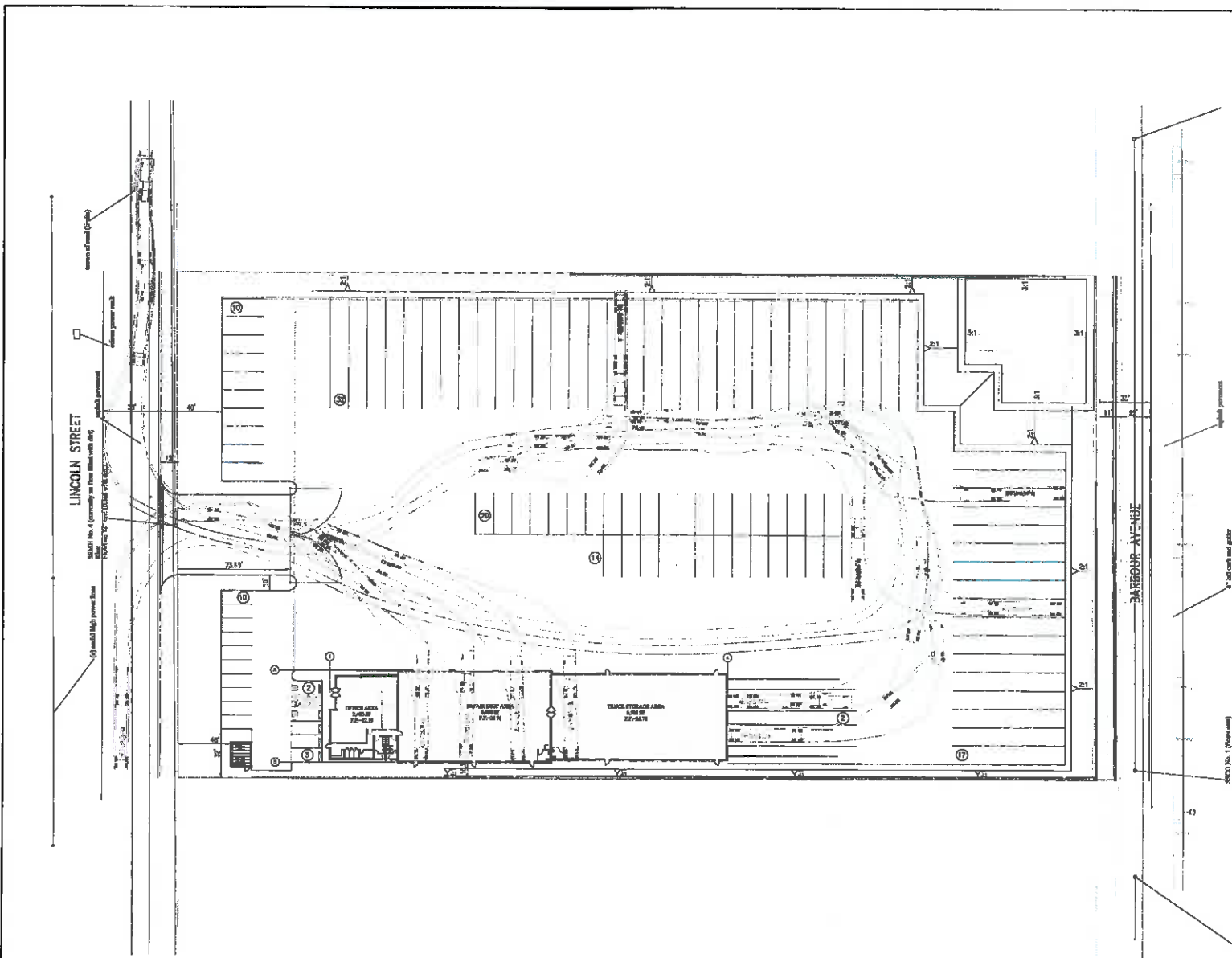
OWNER:  
 ZAKORA LEASE & RENTAL LLC

PLAN DESIGNER:  
 CONTACT: JIMMY LEE

PROJECT NAME:  
**Truck Repair Shop  
 & Truck Parking Terminal**  
 APN: 640-250-002  
 SOUTHWEST PARCEL OF WEST  
 LINCOLN ST. & SOUTH 8TH ST.  
 BANNING, CA 92220

SITEPLAN  
 PROJECT DATA

DATE	11/15/2021
BY	J.L.
CHECKED	J.L.
SCALE	2028
NO.	A0

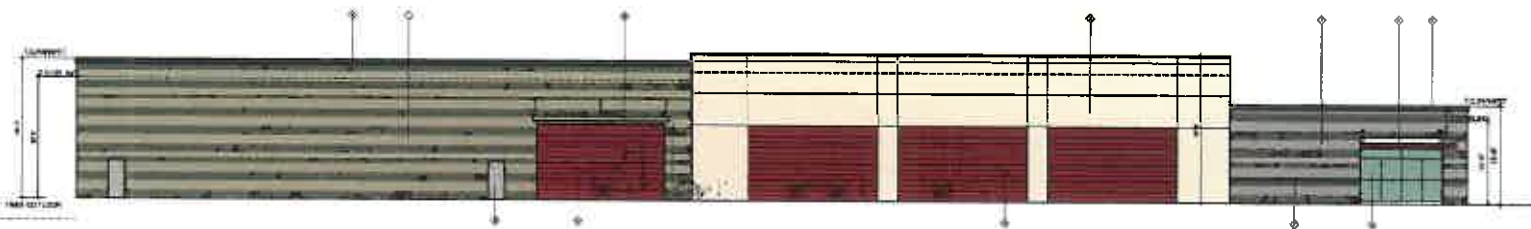


Sheet No. 1 (Sheet 1 of 1)  
 Date: 11/15/21  
 Project: Truck Repair Shop & Truck Parking Terminal

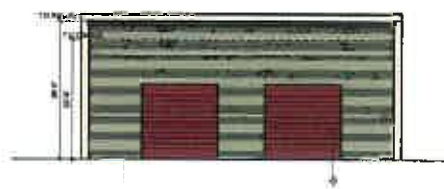
Sheet No. 1 (Sheet 1 of 1)  
 Date: 11/15/21  
 Project: Truck Repair Shop & Truck Parking Terminal



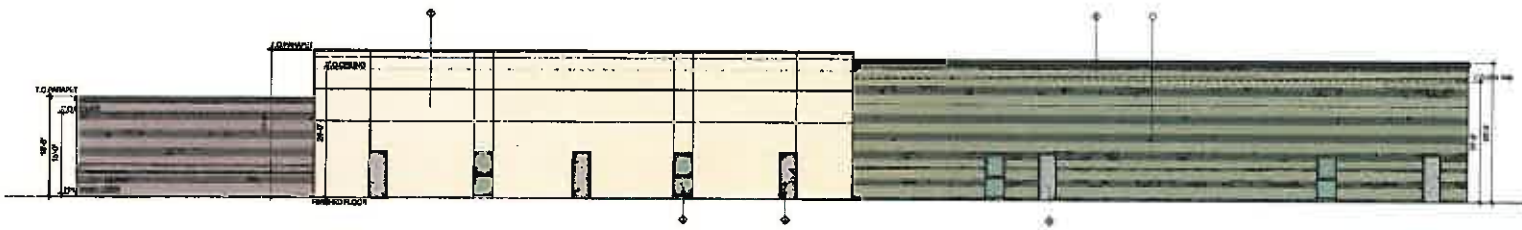
① **BUILDING NORTH ELEVATION**  
 SCALE: 3/32"=1'-0"



② **BUILDING EAST ELEVATION**  
 SCALE: 3/32"=1'-0"



③ **BUILDING SOUTH ELEVATION**  
 SCALE: 3/32"=1'-0"



④ **BUILDING WEST ELEVATION**  
 SCALE: 3/32"=1'-0"



Mail Processing Center  
 Federal Aviation Administration  
 Southwest Regional Office  
 Obstruction Evaluation Group  
 10101 Hillwood Parkway  
 Fort Worth, TX 76177

Aeronautical Study No.  
 2022-AWP-1757-OE

Issued Date: 03/14/2022

William Dean Brown  
 The Planning Consortium  
 29422 Modjeska Canyon Road  
 Silverado, CA 92676

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Multi-purpose Building Truck Repair and Truck Storage Building  
 Location: Banning, CA  
 Latitude: 33-55-15.05N NAD 83  
 Longitude: 116-53-01.20W  
 Heights: 2320 feet site elevation (SE)  
 28 feet above ground level (AGL)  
 2348 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 09/14/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

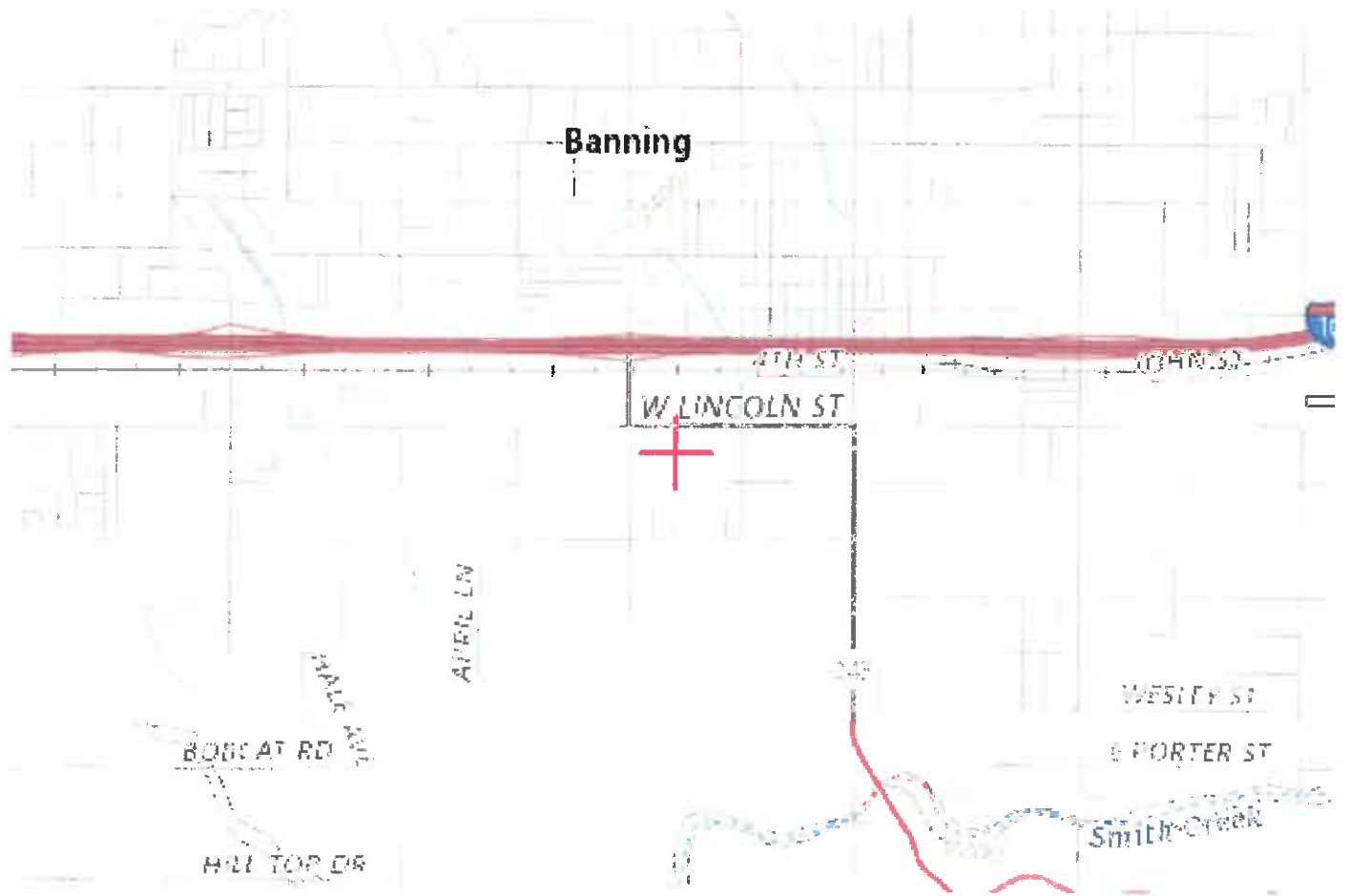
If we can be of further assistance, please contact our office at (206) 231-2877, or [Nicholas.Sanders@faa.gov](mailto:Nicholas.Sanders@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-AWP-1757-OE.

**Signature Control No: 509666154-517756751**

( DNE )

Nicholas Sanders  
Technician

Attachment(s)  
Map(s)



**PAGE BREAK**





# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

April 14, 2022

Kumail Raza, Project Planner  
City of Jurupa Valley Planning Department  
8930 Limonite Avenue  
Jurupa Valley CA 92509

**CHAIR**  
Steven Stewart  
Palm Springs

**VICE CHAIR**  
Steve Manos  
Lake Elsinore

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –  
DIRECTOR’S DETERMINATION**

**COMMISSIONERS**

File No.: ZAP1103RI22  
Related File No.: MA21294 (Plot Plan)  
APNs: 163-021-037

**VACANT**

John Lyon  
Riverside

Russell Betts  
Desert Hot Springs

Richard Stewart  
Moreno Valley

Michael Geller  
Riverside

Dear Mr. Raza

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed City of Jurupa Valley Case No. MA21294 (Plot Plan), a proposal to construct a 7, 380 square foot Auto Zone commercial retail building on 1.37 acres, located on the southwest corner of Limonite Avenue and Corey Street.

**STAFF**

Director  
Paul Rull

Simon A. Housman  
Jackie Vega  
Barbara Santos

The site is located within Airport Compatibility Zone E of the Riverside Municipal Airport Influence Area (AIA). Within Compatibility Zone E of the Riverside Municipal Airport Land Use Compatibility Plan, non-residential intensity is not restricted.

County Administrative Center  
4080 Leron St., 14th Floor.  
Riverside, CA 92501  
(951) 955-5132

The elevation of Runway 9-27 at its westerly terminus is 757.6 feet above mean sea level (AMSL). At a distance of approximately 12,733 feet from the runway to the site, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 884 feet AMSL. The project site elevation is 693 feet AMSL. With a maximum building height of 27 feet, the resulting top point elevation is 720 feet AMSL. Therefore, review of buildings by the FAA Obstruction Evaluation Service (FAAOES) for height/elevation reasons was not required.

[www.rcaluc.org](http://www.rcaluc.org)

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2005 Riverside Municipal Airport Land Use Compatibility Plan, provided that the City of Jurupa Valley applies the following recommended conditions:

**CONDITIONS:**

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses are prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.



- (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, outdoor production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Any use which results in a hazard to flight, including physical (e.g., tall objects), visual, and electronic forms of interference with the safety of aircraft operations.
3. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property.
4. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the stormwater basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at [RCALUC.ORG](http://RCALUC.ORG) which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

If you have any questions, please contact me at (951) 955-6893.

Sincerely,  
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



---

Paul Rull, ALUC Director

Attachments: Notice of Airport in Vicinity

cc: Kimley-Horn and Associates, Inc. (applicant/representative)  
Auto Zone Inc. (property owner)  
Kim Ellis, Airport Manager, Riverside Municipal Airport  
ALUC Case File

X:\AIRPORT CASE FILES\Riverside\ZAP1103RI22\ZAP1103RI22. LTR.doc

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

# NOTICE

**THERE IS AN AIRPORT NEARBY.  
THIS STORM WATER BASIN IS DESIGNED TO HOLD  
STORM WATER FOR ONLY 48 HOURS AND  
NOT TO ATTRACT BIRDS**

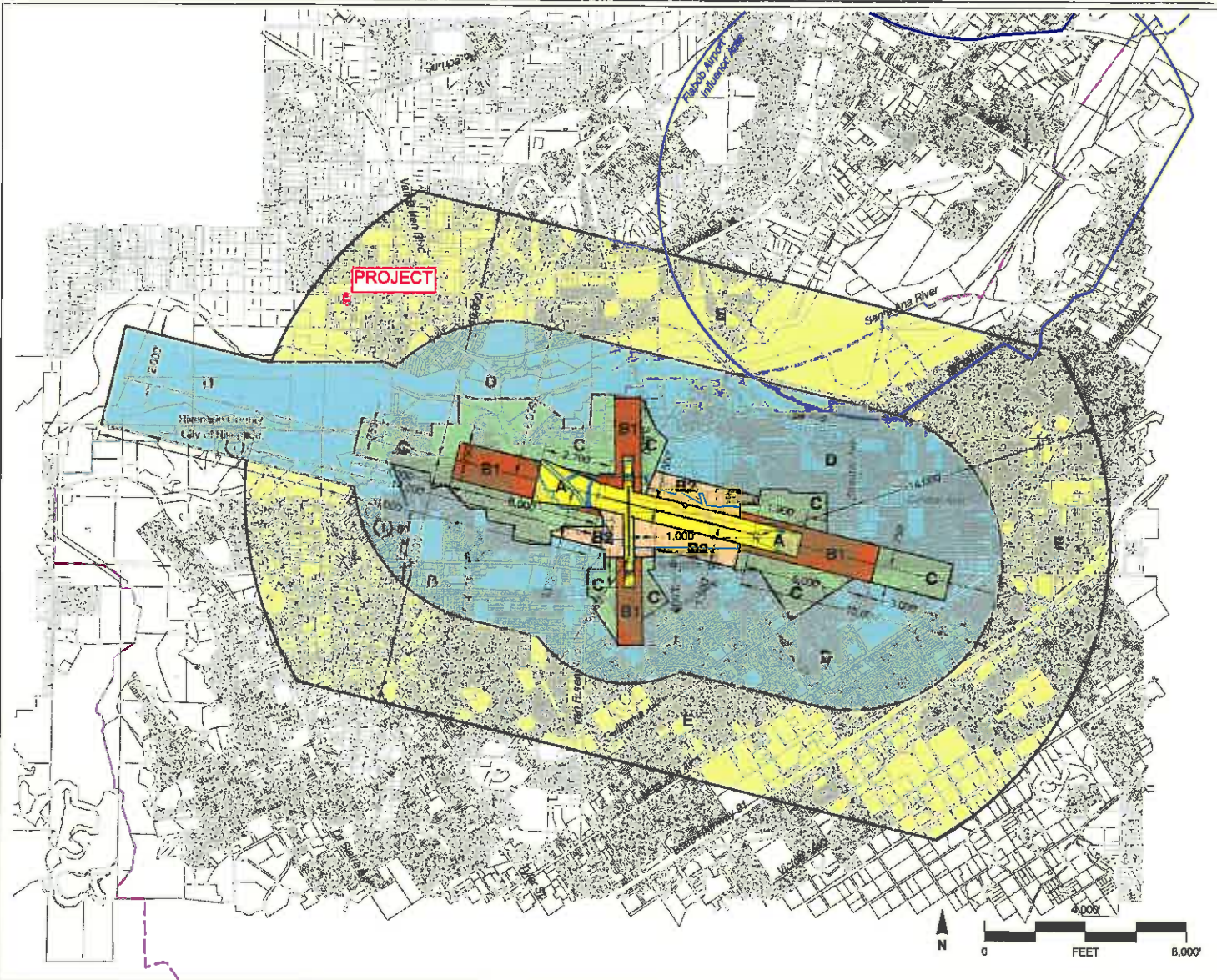
**PROPER MAINTENANCE IS NECESSARY TO AVOID  
BIRD STRIKES**



**IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:**

**Name:** \_\_\_\_\_

**Phone:** \_\_\_\_\_



**Legend**

- Compatibility Zones**
- Airport Influence Area Boundary
  - Zone A
  - Zone B1
  - Zone B2
  - Zone C
  - Zone D
  - Zone E
  - ★ Height Review Overlay Zone

- Boundary Lines**
- Airport Property Line
  - City Limits

**Note**  
 Airport influence boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

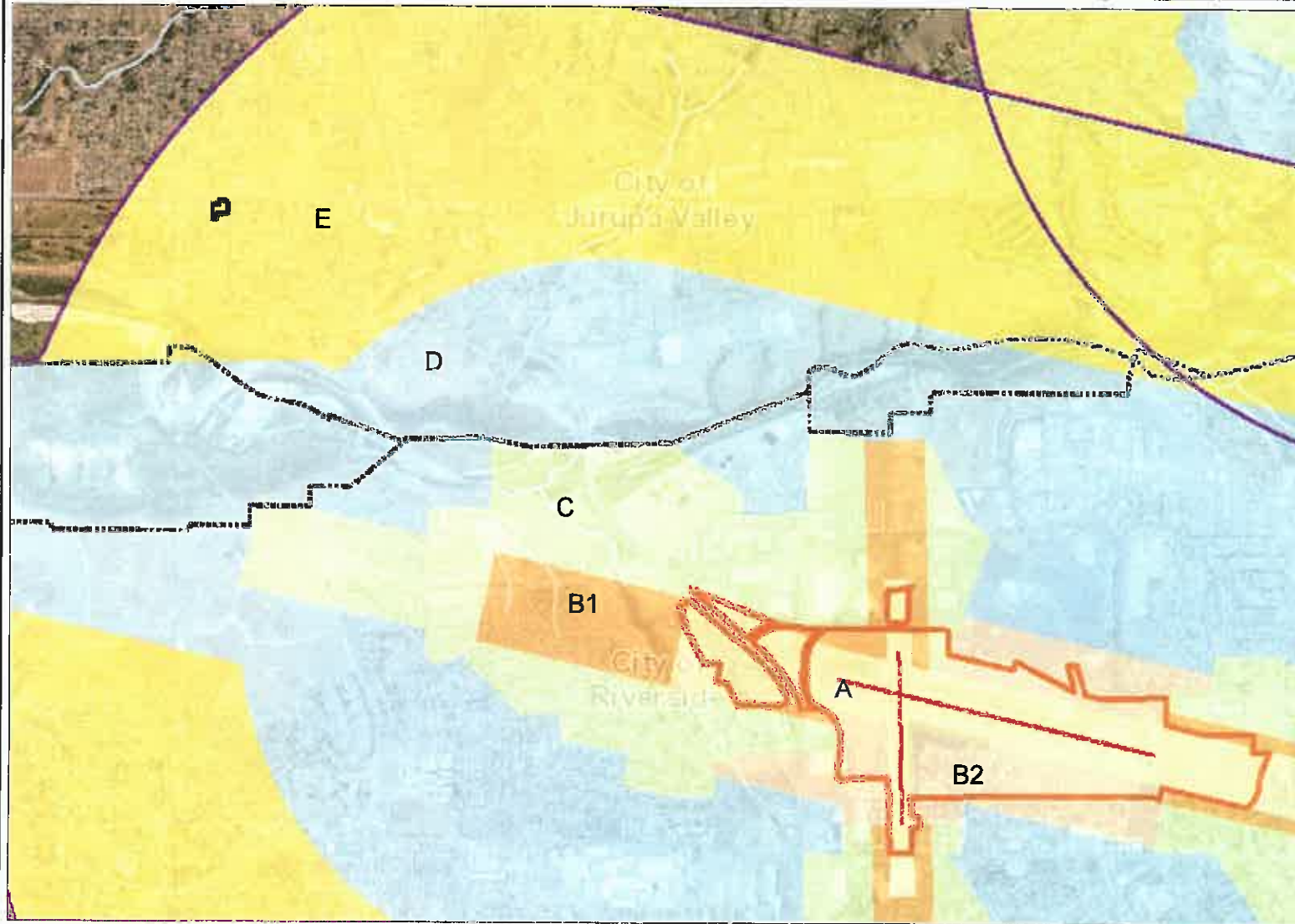
See Chapter 2, Table 2A for compatibility criteria associated with this map. See Section RI.2 for special exceptions to the Table 2A criteria.

**Riverside County**  
**Airport Land Use Commission**  
**Riverside County**  
**Airport Land Use Compatibility Plan**  
**Policy Document**  
*(Adapted March 2005)*

Map RI-1

**Compatibility Map**  
**Riverside Municipal Airport**

# Map My County Map



### Legend

- Runways
- Airports
- Airport Influence Areas

#### Airport Compatibility Zones

- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

### Notes



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# Map My County Map



- Legend**
- Parcels
  - Runways
  - Airports
  - Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
  - A
  - A-EXC1
  - B1
  - B1-APZ I
  - B1-APZ I-EXC1
  - B1-APZ II
  - B1-APZ II-EXC1
  - B1-EXC1
  - B2
  - B2-EXC1
  - C
  - C1
  - C1-EXC1
  - C1-EXC3
  - C1-EXC4
  - C1-HIGHT
  - C2
  - C2-EXC1
  - C2-EXC2
  - C2-EXC3
  - C2-EXC5



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**Notes**



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# Map My County Map



## Legend

- Parcels
- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



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## Notes

0 385 770 Feet

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# Map My County Map



- Legend**
- Blue line Streams
  - Dotted City Areas
  - World Street Map



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**Notes**



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# Map My County Map



## Legend

- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



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## Notes



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# Map My County Map



- Legend**
- Parcels
  - County Centerline Names
  - County Centerlines
  - Blueline Streams
  - City Areas
  - World Street Map



**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

**Notes**

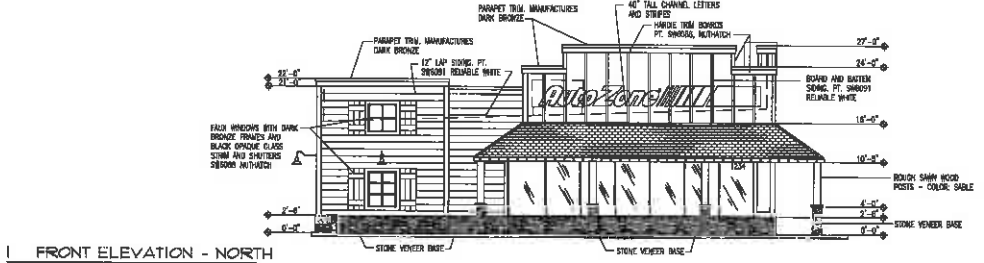


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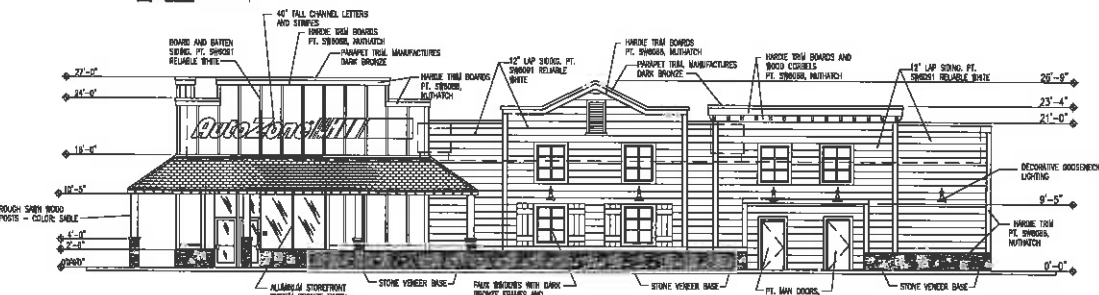
© Riverside County GIS



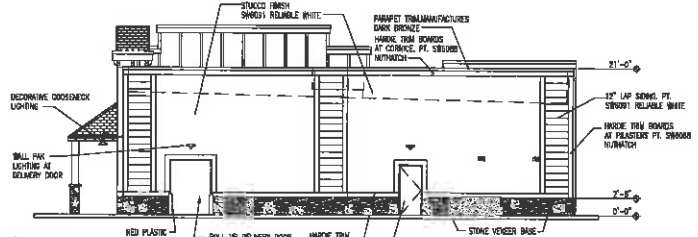
©D. COOPER/2007



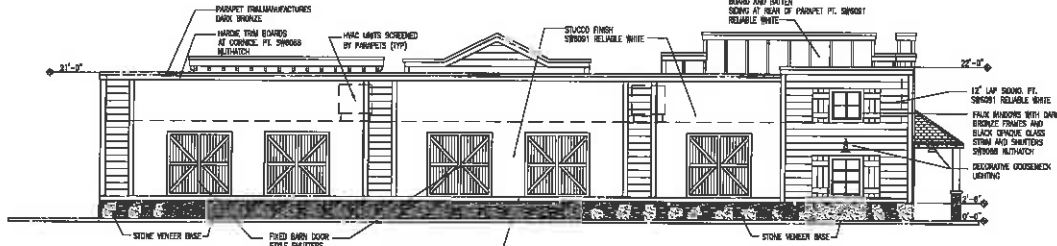
**1 FRONT ELEVATION - NORTH**  
Scale: 1/8" = 1'-0"



**2 RIGHT ELEVATION - WEST**  
Scale: 1/8" = 1'-0"



**3 REAR ELEVATION - SOUTH**  
Scale: 1/8" = 1'-0"



**4 LEFT ELEVATION - EAST**  
Scale: 1/8" = 1'-0"

**MATERIALS AND FINISHES**

- BOARD AND BATTEN SIDING: PT. SHRO01 RELIABLE WHITE
- HARDIE TRIM FASCIA AND CORNICE: PT. SHRO08 NUTMATCH
- STUCCO FINISH: SAND TEXTURE - PT. SHRO03 RELIABLE WHITE
- ROUGH SHAWN POSTS AND BEAMS: PT. SHRO05 SABLE
- FIXED BARN DOOR SHUTTERS: PT. SHRO08 NUTMATCH
- SHINGLE ROOFING: GAF SLATELINE - WEATHERED SLATE
- STORE FRONT SYSTEM: MANUFACTURER'S BRONZE
- STONE VENEER: CULTURED STONE VENEER - LEDGESTONE COLOR: CHARDONEY

REVISIONS	
1	REV1
2	REV2
3	REV3
4	REV4
5	REV5
6	REV6

AutoZone Store No. 3620  
LIMONITE AVE & COREY STREET  
JURUPA VALLEY CA 92509



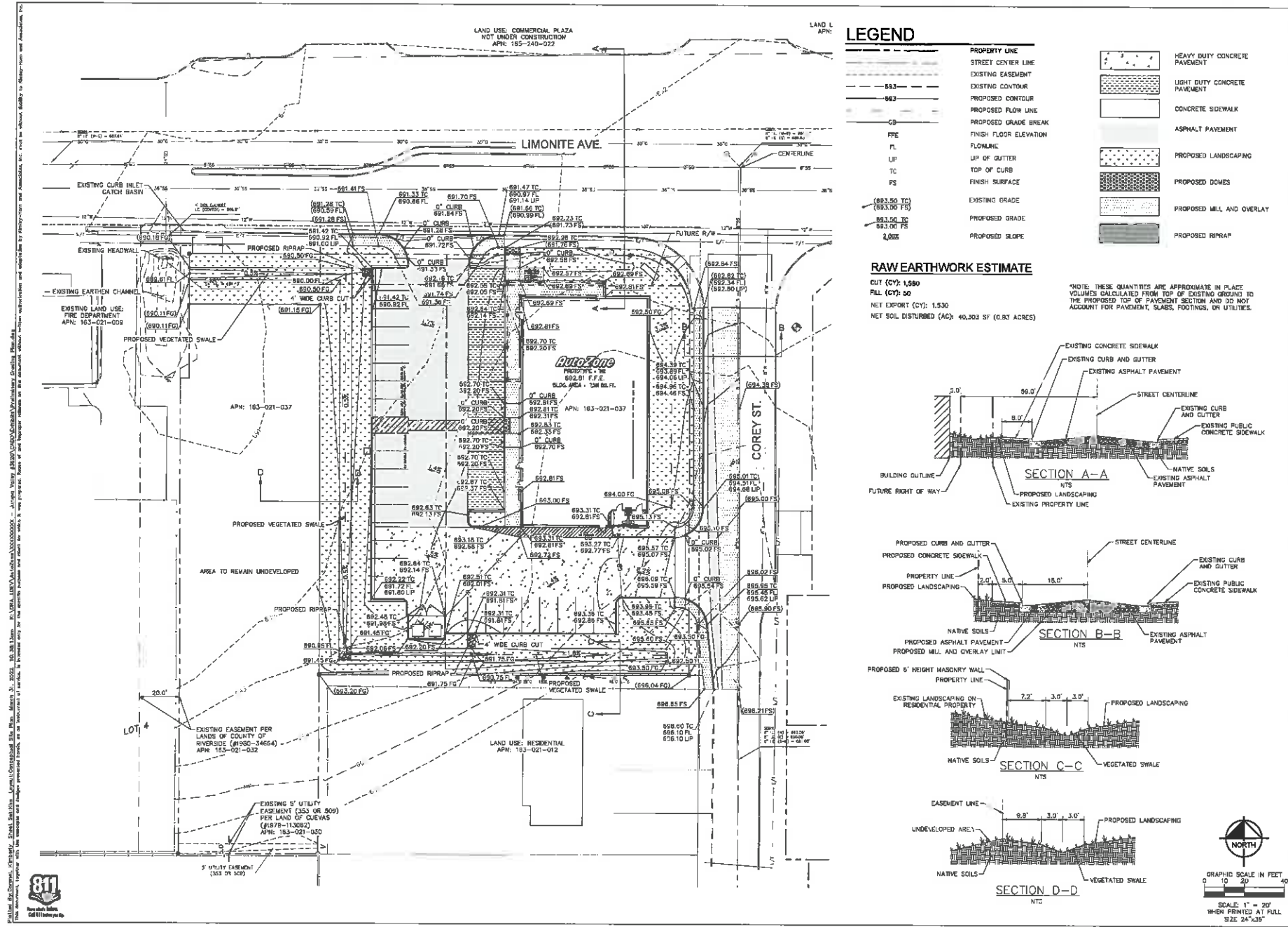
AGI ARCHITECTURAL GROUP INTERNATIONAL  
45 Westwood Drive, Suite 200, Los Angeles, CA 90024  
Tel: 310.440.1111 Fax: 310.440.1112  
www.agi.com

03/15/21

7N2-R

**PS-2**

EXTERIOR ELEVATIONS AND NOTES



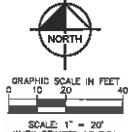
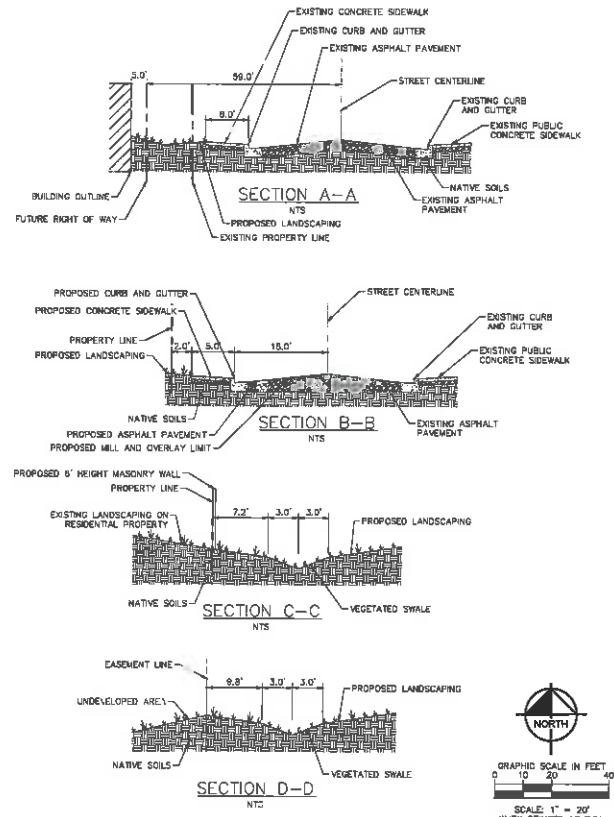
**LEGEND**

- PROPERTY LINE
  - STREET CENTER LINE
  - EXISTING EASEMENT
  - EXISTING CONTOUR
  - PROPOSED CONTOUR
  - PROPOSED FLOW LINE
  - PROPOSED GRADE BREAK
  - FINISH FLOOR ELEVATION
  - FLOWLINE
  - UP OF GUTTER
  - TOP OF CURB
  - FINISH SURFACE
  - EXISTING GRADE
  - PROPOSED GRADE
  - PROPOSED SLOPE
- HEAVY DUTY CONCRETE PAVEMENT
  - LIGHT DUTY CONCRETE PAVEMENT
  - CONCRETE SIDEWALK
  - ASPHALT PAVEMENT
  - PROPOSED LANDSCAPING
  - PROPOSED DOMES
  - PROPOSED MILL AND OVERLAY
  - PROPOSED RIPRAP

**RAW EARTHWORK ESTIMATE**

CUT (CY): 1,580  
 FILL (CY): 90  
 NET EXPORT (CY): 1,530  
 NET SOIL DISTURBED (AC): 40,303 SF (0.83 ACRES)

\*NOTE: THESE QUANTITIES ARE APPROXIMATE IN PLACE VOLUMES CALCULATED FROM TOP OF EXISTING GROUND TO THE PROPOSED TOP OF PAVEMENT SECTION AND DO NOT ACCOUNT FOR PAVEMENT, SLABS, FOOTINGS, OR UTILITIES.



811  
 Autozone  
 1108 W. TOWN AND COUNTRY AVENUE, SUITE 100, JUPITA, FL 32085  
 PREPARED FOR: AUTOZONE  
 PREPARED BY: KIMLEY-HORN AND ASSOCIATES, INC.  
 DATE: 10/20/2022  
 SCALE: 1" = 20'  
 WHEN PRINTED AT FULL SIZE 24"x36"

DATE: \_\_\_\_\_ BY: \_\_\_\_\_

REVISED: \_\_\_\_\_

NO FOR CONSTRUCTION

DATE: \_\_\_\_\_

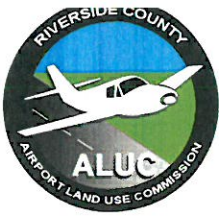
SCALE: AS SHOWN  
 DESIGNED BY: KIC  
 DRAWN BY: KIC  
 CHECKED BY: JAG

PRELIMINARY  
 GRADING AND  
 DRAINAGE PLAN

AUTOZONE 3620  
 PREPARED FOR  
 JURUPA VALLEY, CA

SHEET NUMBER  
**2**

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 1108 W. TOWN AND COUNTRY AVENUE, SUITE 100, JUPITA, FL 32085  
 WWW.KIMLEY-HORN.COM



**AIRPORT LAND USE COMMISSION MEETING  
MINUTES  
April 14, 2022**

**DRAFT**

4-18-22

COMMISSIONERS PRESENT: John Lyon, Steve Manos, Richard Stewart, Larry Smith (alternate for Russell Betts), Maartin Rossouw (alternate for Michael Geller)

COMMISSIONERS ABSENT: Russell Betts, Michael Geller, Steven Stewart

**2.0 PUBLIC HEARING: CONTINUED ITEMS**

NONE

**3.0 PUBLIC HEARING: NEW CASES**

3.1 Staff report recommended: **CONSISTENT**  
Staff recommended at hearing: **CONSISTENT**  
ALUC Commission Action: **CONSISTENT (Vote 5-0; Absent: Steven Stewart)**

**ZAP1510MA22 – Martin Oropeza (Representative: A.K.A. & Associates, Inc)** – County of Riverside Case No. TPM 38067 (Tentative Parcel Map). A proposal to subdivide approximately 2.51 gross acres into 2 single family residential lots, located northerly of Alviso Road, easterly of Day Street, and southerly of Kinney Street (Airport Compatibility Zone C2 of the March Air Reserve Base/Inland Port Airport Influence Area. Staff Planner: Paul Rull at (951) 955-6893, or e-mail at [prull@rivco.org](mailto:prull@rivco.org)

**Motion: John Lyon  
Second: Larry Smith**

3.2 Staff report recommended: **CONSISTENT**  
Staff recommended at hearing: **CONSISTENT**  
ALUC Commission Action: **CONSISTENT (Vote 5-0; Absent: Steven Stewart)**

**ZAP1512MA22 – IPT Riverside Logistics Center II, LLC (Representative: Ares Management, LLC)** – County of Riverside Case No. PPT200002R01 (Plot Plan). A proposal to establish a 307-space truck trailer parking yard on 11.80 acres located on the northwest corner of Placentia Avenue and Harvill Avenue (A proposal to establish an industrial manufacturing building with second floor mezzanine, change of zone, and subsequent plot plan had been previously found consistent by the ALUC) (Airport Compatibility Zones C2 of the March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at [prull@rivco.org](mailto:prull@rivco.org)

**Motion: Richard Stewart  
Second: Maartin Rossouw**

**VIDEO:**

A video recording of the entire proceedings is available on the ALUC website at [www.rcaluc.org](http://www.rcaluc.org). If you have any questions please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at [basantos@rivco.org](mailto:basantos@rivco.org)

**AIRPORT LAND USE COMMISSION MEETING  
MINUTES  
April 14, 2022**

- 3.3 Staff report recommended: **CONSISTENT**  
Staff recommended at hearing: **CONSISTENT**  
ALUC Commission Action: **CONSISTENT (Vote 5-0; Absent: Steven Stewart)**  
**Motion: John Lyon**  
**Second: Martin Rossouw**
- ZAP1511MA22 – Senior Living Riverside, LLC (Representative: Kimley-Horn)** – March Joint Powers Authority Case Nos. GPA20-03 (General Plan Amendment), SP20-03 (Specific Plan Amendment), PP21-07, PP21-08, PP21-09, PP21-10 (Plot Plans), TTM21-02 (Tentative Tract Map No. 38234). A proposal to construct 4 industrial manufacturing buildings with mezzanines on separate parcels totaling 1,203,759 square feet on 64.1 acres located northerly of Nandina Avenue, southerly of 8<sup>th</sup> Street, easterly of Dalla Avenue, and westerly of Village West Drive. The applicant also proposes amending the site's general plan land use designation from Residential to Industrial. The applicant also proposes amending the Air Force Village West Specific Plan to approve a zone change on the site's 64.1 acres from R10 Residential to Industrial, and adopting development standards, design criteria, and master infrastructure plans for the proposed industrial development. The applicant also proposes a tentative tract map to divide 68.83 acres into 4 industrial lots and 1 residential lot (no residential entitlements are proposed at this time) (Airport Compatibility Zone C2 High Terrain Zone of the March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at [prull@rivco.org](mailto:prull@rivco.org)
- 3.4 Staff report recommended: **CONTINUE to 5-12-22**  
Staff recommended at hearing: **CONSISTENT**  
ALUC Commission Action: **CONSISTENT (Vote 5-0; Absent: Steven Stewart)**  
**Motion: Maartin Rossouw**  
**Second: Richard Stewart**
- ZAP1471MA21 – Coudure Family Limited Partnership (Representative: Johnson Aviation, Inc.)** – City of Perris Case Nos. SPA21-05193 (Specific Plan Amendment), DPR21-00011 (Development Plan Review), PLN22-05078 (Tentative Parcel Map No. 38393). A proposal to construct a 231,935 square foot industrial warehouse building with mezzanines on 14.93 acres, located on the northeast corner of Ramona Expressway and Indian Avenue. The applicant also proposes amending the Perris Valley Commerce Center Specific Plan rezoning 17.7 acres from Commercial to Light Industrial. The applicant also proposes a tentative parcel map to divide 14.93 acres into 1 industrial lot and 1 commercial lot (no commercial entitlements are proposed at this time) (Airport Compatibility Zones B1-APZ-II and C1 of the March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at [prull@rivco.org](mailto:prull@rivco.org)
- 3.5 Staff report recommended: **CONSISTENT**  
Staff recommended at hearing: **CONSISTENT**  
ALUC Commission Action: **CONSISTENT (Vote 5-0; Absent: Steven Stewart)**  
**Motion: Richard Stewart**  
**Second: Martin Rossouw**
- ZAP1099PS22 – Farrell Drive, LLC (Representative: Red Rock Realty Investment, LLC)** – City of Palm Springs Case No. 3.4313 (Major Architectural Review). A proposal to construct a 64,583 square foot self-storage facility on 4.20 acres, located northerly of Computer Way, westerly of Research Drive, easterly of Farrell Drive (Airport Compatibility Zone B1 of the Palm Springs International Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at [prull@rivco.org](mailto:prull@rivco.org)

**VIDEO:**

2

A video recording of the entire proceedings is available on the ALUC website at [www.rcaluc.org](http://www.rcaluc.org). If you have any questions please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at [basantos@rivco.org](mailto:basantos@rivco.org)



**AIRPORT LAND USE COMMISSION MEETING  
MINUTES  
April 14, 2022**

**4.0 PUBLIC HEARING: MISCELLANEOUS ITEMS**

None

**5.0 ADMINISTRATIVE ITEMS**

**5.1 Director's Approvals – Information Only**

**5.2 Update March Air Reserve Base Compatibility Use Study (CUS)**

Simon Housman, Project Director March CUS updated the Commission regarding the progress of the March CUS study. There will be several public meetings as we go into the review and adoption process over the next several months. Mr. Housman's best guess now is that we will get to the adoption level sometime around October or November of this year and will be discussed further as time goes on.

**5.3 Chair and Vice Chair Election of Officers for the April meeting**

Larry Smith, alternate for Russell Betts motioned to nominate Steve Manos as the new Chair. Steve Manos, Acting Chair motioned to nominate Russell Betts as the new Vice Chair. The new officers for Chair and Vice Chair will start at the next Commission meeting on May 12. (Vote 5-0; absent Steven Stewart).

**5.4 Commissioners Teleconferencing (Zoom) Requirements**

Ray Mistica, ALUC Counsel presented Power Point slides regarding the Brown Act Teleconferencing Requirements and confirmed direction from Commissioner Manos for ALUC staff to coordinate with the new Chair on issues that may arise in terms of how to handle future teleconference meetings.

**6.0 APPROVAL OF MINUTES**

Commissioner John Lyon motioned to approve the March 10, 2022 minutes. Seconded by Commissioner Richard Stewart. (Vote 5-0; Absent: Steven Stewart)

**7.0 ORAL COMMUNICATION ON ANY MATTER NOT ON THE AGENDA**

Paul Rull, ALUC Director informed the Commission regarding the loss of Airport Land Use Commissioner Art Butler who passed away on March 13, 2022. The ALUC staff has prepared a Certificate of Recognition honoring Mr. Butler's achievements. On behalf on the Riverside County TLMA/ALUC sincere condolences were offered to the Butler family.

**8.0 COMMISSIONER'S COMMENTS**

Larry Smith, alternate for Russell Betts complimented ALUC staff's flexibility and good working relationships with the jurisdictions on new projects.

**9.0 ADJOURNMENT**

Steve Manos, Acting Chair adjourned the meeting at 10:54 a.m. in honor of Commissioner Butler's years of service to the Airport Land Use Commission.

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**VIDEO:**

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A video recording of the entire proceedings is available on the ALUC website at [www.rcaluc.org](http://www.rcaluc.org). If you have any questions please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at [basantos@rivco.org](mailto:basantos@rivco.org)