

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM: 3.4 2.1

HEARING DATE: July 14, 2022 (continued from June 9, 2022)

CASE NUMBER: ZAP1026PV22 – Richland Developers, Inc (Representative: Derek Barbour)

APPROVING JURISDICTION: City of Perris

JURISDICTION CASE NOS: DPR 22-00002 (Development Plan Review), TPM38446 (Tentative Parcel Map)

LAND USE PLAN: 2011 Perris Valley Airport Land Use Compatibility Plan, 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

Airport Influence Area: Perris Valley Airport, March Air Reserve Base/Inland Port Airport

Land Use Policy: Zones B2 and D (Perris Valley); Zone E (March)

Noise Levels: Between 65 - 70 CNEL range from Perris Valley aircraft;
Below 60 CNEL from March aircraft

MAJOR ISSUES: *At the June 9, 2022, hearing, Pat and Melanie Conaster (Airport Manager) commented in opposition to the project, stating concerns with the project's impact on the existing ultralight/light sport aircraft (LSA) runway and its traffic pattern, and skydiving/parachuting business operations. At that meeting, the Commission continued the project to the July 14 meeting to allow staff time to research and analyze these issues.*

Based on ALUC staff's research, it is difficult for the FAA, CALTRANS, and the ALUC (through the ALUCP) to regulate and protect the ultralight runway in its current undesignated status and skydiving operations. Neither the FAA, CALTRANS or the ALUC has any permitting authority over ultralight and skydiving operations. The ultralight runway has not been officially designated by the FAA, and there is no requirement in doing so by the FAA or CALTRANS. The 2011 PVALUCP only identifies runway 15-33 as the officially designated runway where noise contours and safety zones are based on and created. The plan does identify 'ultralight runways' on its Airport Diagram, but it does not contain any runway protection zones. The ultralight runway also operates a significant number of LSA, which is different to ultralights. The FAA does provide runway design standards for LSA, which when applied to the (850 foot length) runway, would require a 1,000 foot (length) runway protection zone. Based on the current ultralight runway and its close proximity to general aviation runway 15-33 air traffic pattern, the runway would not be able to meet the FAA Runway Design Standards for LSA.

Although the proposed project is consistent with the standards and policies as identified in the 2011 PVALUCP, and that the airport manager comments are outside of the purview of the plan, ALUC staff agrees that the safety issues regarding skydiving and ultralight/LSA operations should be further analyzed and evaluated in the project's CEQA process as performed and adopted by the City of Perris.

RECOMMENDATION: Staff recommends that the Development Plan Review and Tentative Parcel Map be found **CONDITIONALLY CONSISTENT** with the 2011 Perris Valley Airport Land Use Compatibility Plan and the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, subject to the conditions included herein, and such additional conditions as may be required by the Federal Aviation Administration Obstruction Evaluation Service. *It is also advised that the local jurisdiction during the CEQA process analyze and evaluate the project's impacts on the safety of the existing skydiving/parachuting and ultralight/light sport aircraft operations.*

PROJECT DESCRIPTION: A proposal to construct two industrial buildings with mezzanines totaling 659,130 square feet on 37.55 acres. The applicant also proposes to divide the site into two parcels.

PROJECT LOCATION: The site is located on the northeast corner of Goetz Road and Mapes Road, within the City of Perris, approximately 701 feet northwesterly of the southeast terminus of Runway 15-33 at Perris Valley Airport.

BACKGROUND:

Perris Valley Airport Manager Opposition Comments: At the June 9, 2022, ALUC hearing, Pat and Melanie Conaster (Airport Manager) provided comments in opposition to the project (their comments were also provided in writing on June 13), stating concerns with the project's impact on the existing ultralight/light sport runway and its traffic pattern, and skydiving/parachuting business operating from the airport. At that meeting, the Commission continued the project to the next public hearing to allow staff time to research and analyze these issues, which are provided below.

- Ultralight/light sport runway - that the existing ultralight/light sport runway and its traffic pattern would be impacted by the project.

According to the Conasters comments, the ultralight/light sport runway is a dirt runway 930 feet long and 110 feet wide, with the centerline approximately 55 feet east of the westerly property line (shared with the proposed project - the edge of the runway is approximately 110 feet from the nearest point of the proposed building). The runway has been in existence for approximately 40 years. The airport is in the process of updating its Airport Facility Directory to include new striping and runway paving per the adopted 2011 Perris Valley Airport Land Use Compatibility Plan (PVALUCP).

- Skydiving/parachuting operations:

According to the Conasters comments, the height and proximity of the proposed building would impact the skydiving and parachuting operations, more specifically parachuting landing areas. They indicated that mechanical turbulence and thermal activity created by the proposed project would create hazards to the skydiving operations by the change in wind direction and velocities. They estimate an average of approximately 120,000 skydiving jumps a year.

In order to address these comments, staff researched the 2011 PVALUCP and the 2011 State Airport Land Use Planning Handbook, contacted Federal Aviation Administration (FAA) and CALTRANS Division of Aeronautics (CALTRANS), and consulted with aviation subject-matter experts Mead & Hunt consultants (M&H). Here are the findings:

1. **2011 PVALUCP.** The plan identifies runway 15-33 as the only officially recognized and designated runway, which is where the noise contours and safety zones are based on and created. No compatibility zones or noise contours were created for the ultralight runway.

The plan does refer to “high volume of ultralight aircraft operations” and the “airport serves as a departure point for jump aircraft and a landing spot for skydivers”, and it also identifies ultralight aircraft in its airport inventory. The plan also identifies the ultralight runway in its Airport Diagram exhibit as 850 feet by 50 feet. No runway protection zones were created at the end of the ultralight runway.

The plan does not contain any specific criteria or policies that deals directly with the ultralight runway or skydiving/parachuting operations, other than “hazards to flight” which is a prohibited use, which is defined as: “hazards to flight include physical (e.g. tall objects), visual, and electronic forms of interference with the safety of aircraft operations”. The proposed building is currently under review with the FAA OES for height obstacle obstruction and will determine the project’s potential impact to air navigation.

2. **2011 State Handbook.** The handbook offers guidance in creating safety compatibility zones which is based on the airport’s type of general aviation runway. The handbook does not provide any guidance on safety compatibility zones for ultralight runways, or to skydiving/parachuting operations.

Based on the handbook’s examples of different types of general aviation runways, the nearest example to the existing ultralight runway (850 feet by 50 feet) is Example 5, low-activity general aviation runway, which assumes less than 2,000 takeoffs and landings per year and the runway length is less than 4,000 feet. More specifically, Example 5 has a Runway Protection Zone (RPZ [Zone A]) length of 1,000 feet, which is greater than the length of the actual ultralight runway (850 feet).

3. **FAA.** The FAA has not recognized the Perris Valley airport ultralight runway as an official designated runway, nor is it recognized as an official flight park.

In order for the airport manager to officially designate/activate the ultralight runway, they would have to file FAA Form 7480-1, which is the notice to file for construction, alternation, and deactivation of airports. The definition “airports” means any landing of takeoff area, which also includes ultralights.

In addition, FAA Circular 103-6 pertains to ultralight vehicle operations, airports, air traffic control, and weather. The FAA does not require any permits for the use of ultralight vehicles, and such vehicles are allowed to use existing airports (at the approval of the airport manager) for take-offs and landings, provided it can be safely operated at the airport along with other flight traffic. The FAA does not have any standards or requirements for the design of ultralight runways.

The FAA does regulate the use and operation of a flight park, which is a site established for the operation of ultralight vehicles. FAA notice to establish a flight park is required under the provisions of Part 157 “notification of construction, alteration, activation, and deactivation of airports” in conjunction with Form 7480-1. The FAA does not have any “standards for the geometric design of an airport built to exclusively serve ultralight vehicles”.

The FAA also has Circular 105-2D, which identifies basic safety requirements for skydiving/parachuting, set forth by the United States Parachute Association. It identifies drop zone requirements as “areas used for skydiving should be unobstructed, with the following radial distances to the nearest obstacle: solo students and A-license holders – 330 feet; B and C license holders and all tandem skydives – 165 feet; and D license holders – 40 feet.

4. **CALTRANS.** CALTRANS identifies ultralight facilities are exempt from State permitting requirements per Public Utilities Code Section 21001, and that ultralight vehicle flight parks are not regulated per PUC Section 21661. Skydiving and parachuting operations are not regulated by CALTRANS.
5. **M&H Aviation Consultant.** The communications with M&H confirmed several of the above findings, including FAA, CALTRANS, and ALUC lack of jurisdiction over ultralight runway and skydiving/parachuting operations.

In addition to those findings above, M&H also raised the point that since ultralight facilities are exempt from CALTRANS requirements, if we were to consider the ultralight runway as a “personal-use” airport, the State Handbook states that compatibility planning falls to the local jurisdiction (City of Perris) for safety issues to be considered.

M&H also mentions that the California Environmental Quality Act (CEQA) requires projects within the vicinity of private airstrips to assess for safety hazards. Based on the findings above identifying that ALUC does not have jurisdiction in regulating the ultralight and skydiving activities, that the CEQA analysis performed by the local jurisdiction can address the concerns raised by the airport manager.

6. **Ultralight and Sport Pilots of America Aviation Club.** The Club’s website provides an illustration of the traffic pattern from the ultralight runway showing arrivals and departures at the southerly end of the runway, with flight traffic patterns occurring further south and to the east. According to this exhibit, the identified traffic pattern does not extend northerly beyond Mapes Road, which is where the project is located.
7. **Light Sport Aircraft (LSA).** As identified in the Conaster’s comments, the ultralight runway is also utilized by LSA. LSA are generally heavier, faster, and have greater aeronautical capabilities than ultralights. FAA Order 8130.2, Airworthiness Certification of Aircraft, establishes policies and procedures for LSA. The 2011 PVALUCP does not identify or reference the operations of LSA.

FAA Advisory Circular 150/5300-13B regulates LSA under the standard for “small aircraft with approach speeds of more than 50 knots”. The circular also provides basic Runway Design Standards for these small aircraft (LSA) including a runway

protection zone (RPZ) length of 1,000 feet (and a runway safety area length of 300 feet based on visual visibility minimums) which is longer than the actual length of the runway (850 feet).

Based on the current ultralight runway and its close proximity to general aviation runway 15-33 air traffic pattern, the runway would not be able to meet the FAA Runway Design Standards for LSA.

Based on the above findings, ALUC staff is providing the following conclusions:

- It is difficult for the FAA, CALTRANS, and ALUC (through the ALUCP) to regulate and protect the ultralight runway in its current undesignated status and skydiving operations. Also, the State handbook does not provide any guidance on safety compatibility zones for ultralight runways, or to skydiving/parachuting operations.
- As it stands, the ultralight runway is technically not a runway as it has not been designated/activated by the FAA (and there is no requirement to do so by the FAA or CALTRANS). As such, if we were to consider the ultralight facility as a “personal-use” airport, the state handbook says that compatibility planning falls to the local jurisdiction (City of Perris) with regards to considering safety issues.
- Based on the Aviation Club ultralight runway traffic pattern, aircraft flight paths do not occur over the project site.
- Based on the LSA discussion above, the ultralight “runway” is more accurately identified as an unpermitted LSA runway, which in its current setting and its close proximity to runway 15-33, would not meet FAA Runway Design standards for LSA.
- The CEQA process that applies to this project is required to analyze and evaluate safety impacts by the local jurisdiction. Staff recommends that a wind analysis be performed to assess the safety impacts to parachute operations, as well as ultralight activity, and that the applicant explore the potential of applying the handbook safety zones to the ultralight runway in order to keep the project outside of high-risk zones. Therefore, it is reasonable to assume that with the cooperation of the applicant, airport manager, and the City of Perris, that these issues expressed by the airport manager will be analyzed and evaluated during the CEQA process.

Non-Residential Average Land Use Intensity: Pursuant to the Perris Valley Airport Land Use Compatibility Plan, the project boundary is located within Zones B2 (13.85 acres) and D (23.48 acres), which limits average intensity to 100 people each. The project is also located in Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, which does not restrict non-residential intensity.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, the following rates were used to calculate the occupancy for the proposed buildings:

- Office – 1 person per 200 square feet
- Warehouse – 1 person per 500 square feet

The project proposes to construct 2 industrial buildings with mezzanines totaling 659,130 square feet (on separate parcels) on 37.55 acres. An individual lot-by-lot analysis is included below:

- Building 1, Parcel 1 (24.50 acres) is located within Zones B2 (13.36 acres) and D (11.34 acres) and proposes a 502,590 square foot industrial warehouse building with mezzanines, which includes in Zone B2 200,725 square feet of warehouse area, accommodating an occupancy of 401 people, and resulting in an average intensity of 30 people per acre, which is consistent with the Compatibility Zone B2 average intensity criterion of 100 people per acre. The portion of the building located within Zone D consist of 288,649 square feet of warehouse area, 5,000 square feet of first floor office area, and 5,000 square feet of second floor office mezzanine area, accommodating an occupancy of 627 people, and resulting in an average intensity of 55 people per acre, which is consistent with Compatibility Zone D average intensity criterion of 100 people per acre.
- Building 2, Parcel 2 (12.12 acres) is located within Zones B2 (0.55 acres) and D (11.57 acres) and proposes a 146,040 square foot industrial warehouse building with mezzanines (located entirely within Zone D, no buildings are proposed within Zone B2 portion of parcel 2), which includes 136,040 square feet of warehouse area, 5,000 square feet of first floor office area, and 5,000 square feet of second floor office mezzanine area, accommodating an occupancy of 302 people, and resulting in an average intensity of 26 people per acre, which is consistent with the Compatibility Zone D average intensity criterion of 100 people per acre.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle and 1.0 persons per trailer truck space). An individual lot-by-lot analysis is included below:

- Building 1 Parcel 1 includes 395 standard vehicles and 96 trailer spaces, accommodating a total occupancy of 491 people, resulting in an average intensity of 20 people per acre, which is consistent with the Compatibility Zone B2 and D average intensity criterion of 100 people per acre.
- Building 2 Parcel 2 includes 137 standard vehicles and 47 trailer spaces, accommodating a total occupancy of 184 people, resulting in an average intensity of 15 people per acre, which is consistent with the Compatibility Zone B2 and D average intensity criterion of 100 people per acre.

Non-Residential Single-Acre Intensity: Pursuant to the Perris Valley Airport Land Use Compatibility Plan, the project boundary is located within Zones B2 (13.85 acres) and D (23.48 acres), which limits single acre intensity to 200 people and 300 people respectively. The project is also located within March Air Reserve Base/Inland Port Airport Zone E, which does not restrict non-residential intensity.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre area for each of the buildings are as follows:

- Building 1 single acre intensity located in Zone B2 includes 43,560 square feet of warehouse building, resulting in single acre intensity of 87 people, consistent with the Compatibility Zone B2 single acre intensity criterion maximum of 200 people.

Building 1 single acre intensity located in Zone D includes 33,560 square feet of warehouse area, 5,000 square feet of first floor office area, 5,000 square feet of second floor office

mezzanine area, resulting in a single acre intensity of 117 people, which is consistent with the Compatibility Zone D single acre intensity criterion maximum of 300 people.

- Building 2 single acre intensity located in Zone D includes 33,560 square feet of warehouse area, 5,000 square feet of first floor office area, 5,000 square feet of second floor office mezzanine area, resulting in a single acre intensity of 117 people, which is consistent with the Compatibility Zone D single acre intensity criterion maximum of 300 people.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zone B2, D or E.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being in an area below the 60 CNEL range from aircraft noise. The Perris Valley Airport Land Use Compatibility Plan depicts the site as being affected by aircraft noise of 65 - 70 CNEL. As a primarily industrial use not sensitive to noise (and considering typical anticipated building construction noise attenuation of approximately 20 dBA), the warehouse area would not require special measures to mitigate aircraft-generated noise. However, a condition is included to provide for adequate noise attenuation within office areas of the buildings.

Part 77: The elevation of Perris Valley Airport's Runway 15-33 at its southeasterly terminus is 1,413 feet above mean sea level (AMSL). At a distance of approximately 701 feet from the runway to the site, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,420 feet AMSL. The maximum finished floor elevation is 1,422 feet AMSL and the maximum building height is 50 feet, resulting in a top point elevation of 1,472 feet AMSL. Therefore, review of this building by the FAA Obstruction Evaluation Service (FAA OES) is required. The applicant has submitted Form 7460-1, and FAA OES has assigned Aeronautical Study No. 2022-AWP-8356-OE to this project. Its status is currently a "work in progress".

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33C). The project is located 701 feet from the runway, and therefore would be subject to the above requirement.

The project includes a 50,000 square foot bioretention basin in Zone D and therefore has the potential to provide food, water, and shelter for hazardous wildlife. Pursuant to the study "Wildlife Hazard Management at Riverside County Airports: Background and Policy", October 2018, by Mead & Hunt, which is the basis of the brochure titled "Airports, Wildlife and Stormwater Management", such basins are permissible in Zone D when vegetation is selected careful so as not to provide food, shelter, nesting, roosting, or water for wildlife. The project has been conditioned to be consistent with the basin criteria (as well as providing 48-hour draw down of the basin).

Open Area: The project site is split between Compatibility Zones B2 (13.85 acres) and D (23.48 acres). Compatibility Zone D requires 10% (2.35 acres) of the land area within major projects (10 acres or larger) be set aside as open area that could potentially serve as emergency landing areas (Compatibility Zone B2 does not require any amount of open area). The project provides 2.41 acres of ALUC eligible open areas consisting of driveway aisles and parking lot areas within the proposed

development. The project is conditioned to maintain these areas consistent with ALUC open area requirements of 300 feet by 75 feet minimum shape and prohibit obstructions greater than 4 feet in height that are at least 4 inches in diameter.

CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, outdoor production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, day care centers, libraries, hospitals, nursing homes, places of worship, buildings with more than two aboveground habitable floors, critical community infrastructure facilities, and aboveground bulk storage of 6,000 gallons or more of flammable or hazardous materials.
 - (f) Highly noise-sensitive outdoor nonresidential uses.
 - (g) Any use which results in a hazard to flight, including physical (e.g., tall objects), visual, and electronic forms of interference with the safety of aircraft operations.
3. Prior to issuance of building permits, the property owner shall convey an avigation easement to the Perris Valley Airport. Copies of the recorded avigation easement shall be forwarded to the Airport Land Use Commission and to the City of Perris.
4. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property and be recorded as a deed notice.
5. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry

between rainfalls. Vegetation in and around the basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the stormwater basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

6. This project has been evaluated as consisting of 10,000 square feet of first floor office area, 10,000 square feet of second floor mezzanine office area, and 630,280 square feet of warehouse area. Any increase in building area, height, change in use to any higher intensity use, change in building location or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.
7. Noise attenuation measures shall be incorporated into the design of the office area, to the extent such measures are necessary to ensure that interior noise levels from aircraft operations are at or below 45 CNEL.
8. At least 2.35 acres of ALUC-eligible open areas (at least 75 feet in width and 300 feet in length), as depicted on the Open Space exhibit, shall be kept obstacle and obstruction free per ALUC open area definition (no objects greater than four feet in height with a diameter of four inches or greater).
9. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission and Riverside County as owner and operator of French Valley Airport. In the event of any reasonable complaint about glare related to aircraft operations, the applicant shall agree to such specific mitigation measures as determined or requested by Riverside County.

Rull, Paul



From: skydiveperrispat@aol.com
Sent: Monday, June 13, 2022 12:30 PM
To: Rull, Paul
Cc: melanie@skydiveperris.com; michael.smith@dot.ca.gov
Subject: Re: ZAP1026PV22 (Richland Developers) ALUC review
Attachments: L65 Ultralight-Light Sport Runway.pdf

Good Morning Mr. Rull, based upon my sister 's and my comments at the June 9 2022 meeting we are opposed to the proposed project due to safety concerns in it's current rendition. I will try and recap what we said and if there are any differences I apologize in advance. My sister and business partner Melanie Conatser may add to our response if she deems necessary, she is currently out of town. I initially spoke in regards to the overall scope, size, and proximity of the project being too big and too close in my mind to L65. The first major concern is our Ultralight/Light Sport runway and the traffic pattern. The traffic pattern currently is directly over and thru what would be the future project. Right Hand for Runway 17 and Left hand for Runway 35. I know the question was brought to me regarding distance and locations so I added a Google Earth picture with the runway depicted, see attached. Basically the runway is a 350/170 dirt runway 930' long and 110' wide with the centerline approximately 55' from our southwest property line. This runway has been in existence for approximately 40 years. Although the ultralight runway was not added specifically to the compatibility study it was referenced in Vol 2 of the 2011 L65 approved plan. I have spoke with Mr. Mike Smith with California Division of Aeronautics as we are in the process of updating the AFD in regards to new striping and runway paving per the approved 2011 plan and hopefully the ultralight runway will be depicted. The ultralights were always acknowledged but the runway not depicted. The second major concern that my sister brought up was the height and proximity to our parachute landing areas. As proposed would potentially cause hazardous mechanical turbulence and thermal activity due to heights and color background changes from native. She noted that unlike airplanes, parachutes are unpowered and much less forgiving to changes in wind direction and velocities, which will all be effected. My sister's back ground is she is a multiple skydiving world record holder with a pro rating and approximately 6000 jumps. My background as a pilot is Multi Commercial, ATP, FE, A&P, IA with approximately 4000 hours pic and 7500 total time with probably 3000+ out of Perris. The Perris Airport has been in operation since 1933 being used in many ways including as an alternate glider landing strip during WW2. There have been many changes over the years, at one time it even had a cross runway running east/west. We currently operate one of if not the largest parachute drop zones in the world working with many domestic and foreign military's training in all aspects. Our current average jumps per year are approximately 120.000 so you can understand why we should be concerned. I greatly appreciate what you do and any help in this matter is appreciated. If possible any continuances would be appreciated as this project was just brought to us last week and are trying to provide any additional information we can. Thanks again Patrick & Melanie Conatser Perris Valley Airport 951-203-5668 anytime

L65 Ultralight Runway

Write a description for your map.

Legend

-  AJ Rental Perris
 -  Perris Valley Aviation
- 6/13/2022

Proposed Project
20219

ULTRALIGHT

Perris Valley Aviation

33

Mapes Rd

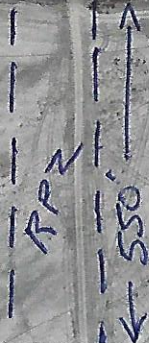
Mapes Rd

L65 ULTRALIGHT Runway
930' L 350°/170° x 110' W

Runway @ 55' FROM WEST
PROPERTY LINE

Google Earth

RTA TRAFFIC 17 / UL TRAFFIC 35



800 ft

2.10.2 Light Sport Aircraft (LSA).

LSA generally have greater aeronautical capabilities than ultralights, while being lighter than GA aircraft certified under 14 CFR Part 23, *Airworthiness Standards: Normal Category Airplanes*, and 14 CFR Part 27, *Airworthiness Standards: Normal Category Rotorcraft*. FAA Order 8130.2, *Airworthiness Certification of Aircraft*, establishes policies and procedures for LSA. The FAA bases certification of LSAs on accepted industry consensus standards. For aircraft operating under LSA rules, use the standards in this AC for small aircraft with approach speeds of more than 50 knots.

2.10.3 Ultralights.

Title 14 CFR Part 103, *Ultralight Vehicles*, regulates ultralight aircraft. Maximum takeoff weight is less than 254 lbs (115 kg) and maximum stall speed is not more than 24 knots. Use the standards in this AC for small aircraft with approach speeds of less than 50 knots. Refer to AC 103-6 for operational guidance.

2.10.4 Seaplanes.

Refer to AC 150/5395-1.

2.10.5 Parachute Operations.

Parachute operations are a permissible aeronautical activity at Federally-obligated airports subject to compliance with reasonable terms and regulatory requirements. Per 14 CFR Part 105, *Parachute Operations*, parachute operations on an airport require prior approval from the airport operator. Refer to FAA Order 5190.6 for FAA policy addressing reasonable accommodation of parachute operations at an airport, safety considerations, and coordination with other appropriate FAA offices. Additional resources for information on parachute operations as it relates to airport design include:

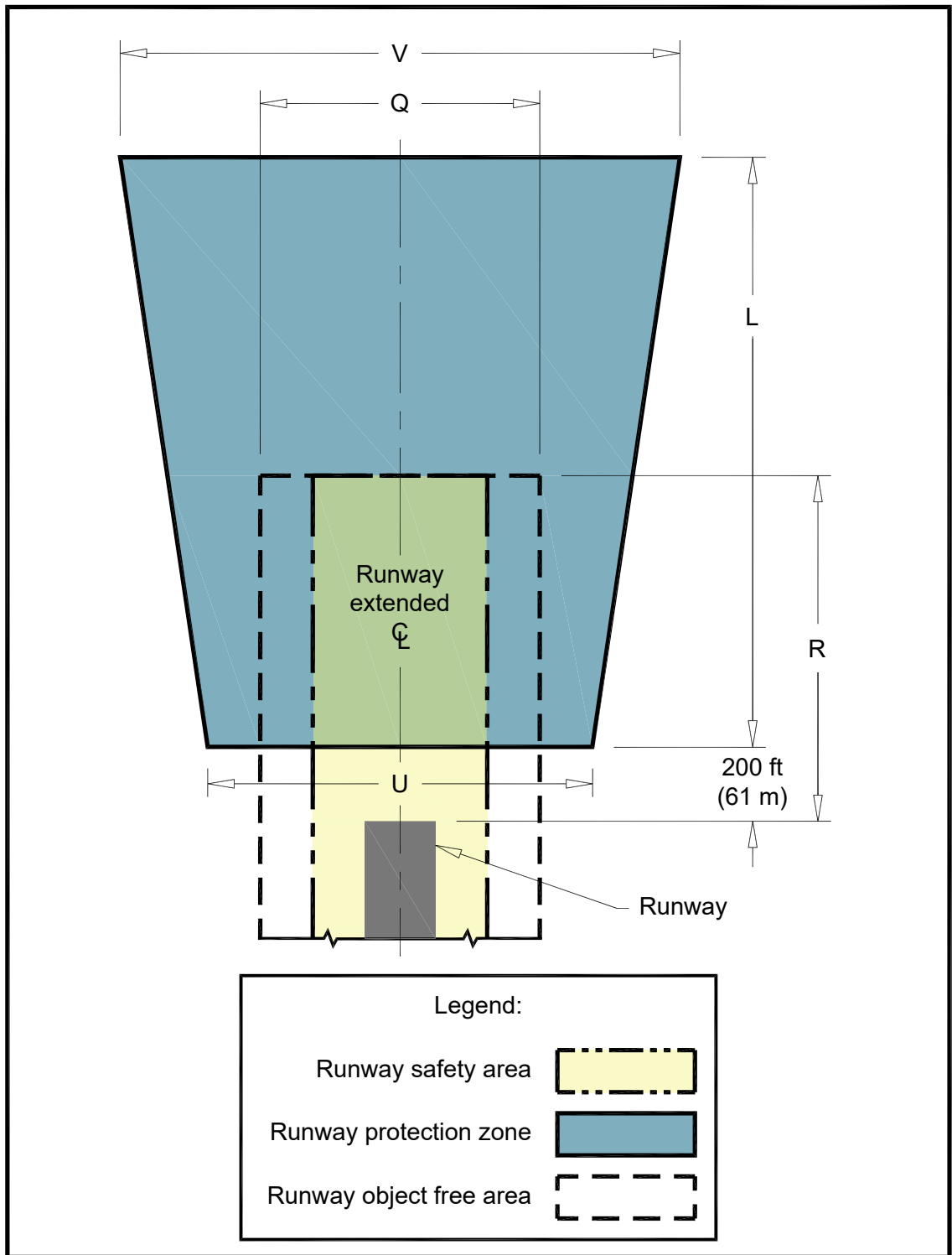
- FAA Order 7110.65, *Air Traffic Control*.
- FAA Order 7210.3, *Facility Operation and Administration*.
- AC 150/5190-7, *Minimum Standards for Commercial Aeronautical Activities*.
- AC 105-2, *Sport Parachuting*.
- AC 90-66, *Non-Towered Airport Flight Operations*.
- United States Parachute Association (USPA), *Basic Safety Requirements (BSR)*.

2.10.6 Aircraft Operations in the Unpaved Runway Safety Area (RSA).

The primary function of a standard RSA is to enhance the safety of aircraft that undershoot, overrun, or veer off the runway. Pilots of certain aircraft (such as ultralights, powered-parachutes, helicopters, gliders, agricultural aircraft, tailwheels, aircraft with large balloon type “tundra” tires, etc.) occasionally use the unpaved portion of the RSA adjacent to a runway for takeoffs, landings, or other operations (e.g., banner towing). While aircraft operations from the unpaved portion of an RSA are not inherently unsafe, such operations have the potential to introduce various hazards and risks to the pilot, as well as other aircraft, vehicles, individuals, and facilities on the airport.

2.10.6.1 Key risk factors to consider include:

Figure 3-26. Runway Protection Zone (RPZ), Runway Object Free Area (ROFA), and Runway Safety Area (RSA)



Note: See [Appendix G](#) or online [Runway Design Standards Matrix Tool](#) for dimensions.

Table G-3. Runway Design Standards Matrix, A/B-II Small Aircraft

<i>Aircraft Approach Category (AAC) and Airplane Design Group (ADG):</i>		A/B – II Small Aircraft			
ITEM	DIM 1	VISIBILITY MINIMUMS			
		Visual	Not Lower than 1 mile	Not Lower than 3/4 mile	Lower than 3/4 mile
RUNWAY DESIGN					
Runway Length	A	<i>Refer to paragraphs 3.3 and 3.7.1</i>			
Runway Width	B	75 ft	75 ft	75 ft	100 ft
Shoulder Width		10 ft	10 ft	10 ft	10 ft
Blast Pad Width		95 ft	95 ft	95 ft	120 ft
Blast Pad Length		150 ft	150 ft	150 ft	150 ft
Crosswind Component		13 knots	13 knots	13 knots	13 knots
RUNWAY PROTECTION					
Runway Safety Area (RSA)					
Length beyond departure end ^{9, 10}	R	300 ft	300 ft	300 ft	600 ft
Length prior to threshold	P	300 ft	300 ft	300 ft	600 ft
Width	C	150 ft	150 ft	150 ft	300 ft
Runway Object Free Area (ROFA)					
Length beyond runway end	R	300 ft	300 ft	300 ft	600 ft
Length prior to threshold	P	300 ft	300 ft	300 ft	600 ft
Width	Q	500 ft	500 ft	500 ft	800 ft
Obstacle Free Zone (OFZ)					
Runway, Inner-approach, Inner- Transitional		<i>Refer to paragraph 3.11</i>			
Precision Obstacle Free Zone (POFZ)					
Length		N/A	N/A	N/A	200 ft
Width		N/A	N/A	N/A	800 ft
Approach Runway Protection Zone (RPZ)					
Length	L	1,000 ft	1,000 ft	1,700 ft	2,500 ft
Inner Width	U	250 ft	250 ft	1,000 ft	1,000 ft
Outer Width	V	450 ft	450 ft	1,510 ft	1,750 ft
Departure Runway Protection Zone (RPZ)					
Length	L	1,000 ft	1,000 ft	1,000 ft	1,000 ft
Inner Width	U	250 ft	250 ft	500 ft	500 ft
Outer Width	V	450 ft	450 ft	700 ft	700 ft
RUNWAY SEPARATION					
<i>Runway centerline to:</i>					
Parallel runway centerline	H	<i>Refer to paragraph 3.9</i>			
Holding Position		125 ft	125 ft	125 ft	175 ft
Parallel taxiway/taxilane centerline ^{2, 4}	D	240 ft	240 ft	240 ft	300 ft
Aircraft parking area	G	<i>Refer to paragraph 5.4.1.2</i>			
Helicopter touchdown pad		<i>Refer to AC 150/5390-2</i>			

Note: Values in the table are rounded to the nearest foot. 1 foot = 0.305 meters.

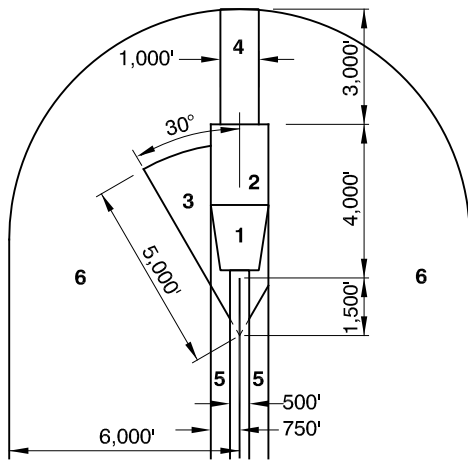
Note: See the Footnotes on the page after Table G-12.

Table G-4. Runway Design Standards Matrix, A/B-II

<i>Aircraft Approach Category (AAC) and Airplane Design Group (ADG):</i>		A/B – II			
ITEM	DIM 1	VISIBILITY MINIMUMS			
		Visual	Not Lower than 1 mile	Not Lower than 3/4 mile	Lower than 3/4 mile
RUNWAY DESIGN					
Runway Length	A	<i>Refer to paragraphs 3.3 and 3.7.1</i>			
Runway Width	B	75 ft	75 ft	75 ft	100 ft
Shoulder Width		10 ft	10 ft	10 ft	10 ft
Blast Pad Width		95 ft	95 ft	95 ft	120 ft
Blast Pad Length		150 ft	150 ft	150 ft	150 ft
Crosswind Component		13 knots	13 knots	13 knots	13 knots
RUNWAY PROTECTION					
Runway Safety Area (RSA)					
Length beyond departure end ^{9, 10}	R	300 ft	300 ft	300 ft	600 ft
Length prior to threshold	P	300 ft	300 ft	300 ft	600 ft
Width	C	150 ft	150 ft	150 ft	300 ft
Runway Object Free Area (ROFA)					
Length beyond runway end	R	300 ft	300 ft	300 ft	600 ft
Length prior to threshold	P	300 ft	300 ft	300 ft	600 ft
Width	Q	500 ft	500 ft	500 ft	800 ft
Obstacle Free Zone (OFZ)					
Runway, Inner-approach, Inner- Transitional		<i>Refer to paragraph 3.11</i>			
Precision Obstacle Free Zone (POFZ)					
Length		N/A	N/A	N/A	200 ft
Width		N/A	N/A	N/A	800 ft
Approach Runway Protection Zone (RPZ)					
Length	L	1,000 ft	1,000 ft	1,700 ft	2,500 ft
Inner Width	U	500 ft	500 ft	1,000 ft	1,000 ft
Outer Width	V	700 ft	700 ft	1,510 ft	1,750 ft
Departure Runway Protection Zone (RPZ)					
Length	L	1,000 ft	1,000 ft	1,000 ft	1,000 ft
Inner Width	U	500 ft	500 ft	500 ft	500 ft
Outer Width	V	700 ft	700 ft	700 ft	700 ft
RUNWAY SEPARATION					
<i>Runway centerline to:</i>					
Parallel runway centerline	H	<i>Refer to paragraph 3.9</i>			
Holding Position		200 ft	200 ft	200 ft	250 ft
Parallel taxiway/taxilane centerline ^{2, 4}	D	240 ft	240 ft	240 ft	300 ft
Aircraft parking area	G	<i>Refer to paragraph 5.4.1.2</i>			
Helicopter touchdown pad		<i>Refer to AC 150/5390-2</i>			

Note: Values in the table are rounded to the nearest foot. 1 foot = 0.305 meters.

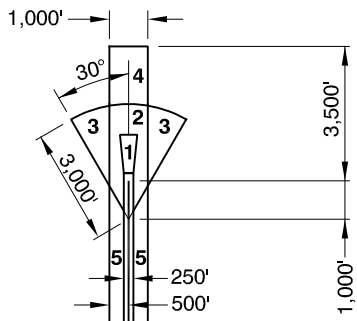
Note: See the Footnotes on the page after Table G-12.



**Example 4:
General Aviation Runway with
Single-Sided Traffic Pattern**

Assumptions:

- No traffic pattern on right
 - Length 4,000 to 5,999 feet
 - Approach visibility minimums \geq 3/4 mile and $<$ 1 mile
 - Zone 1 = 1,000' x 1,510' x 1,700'
- See Note 1.



**Example 5:
Low-Activity General Aviation Runway**

Assumptions:

- Less than 2,000 takeoffs and landings per year at individual runway end.
 - Length less than 4,000 feet
 - Approach visibility minimums \geq 1 mile or visual approach only
 - Zone 1 = 250' x 450' x 1,000'
- See Note 1.

Legend

1. Runway Protection Zone
2. Inner Approach/Departure Zone
3. Inner Turning Zone
4. Outer Approach/Departure Zone
5. Sideline Zone
6. Traffic Pattern Zone

Notes:

1. RPZ (Zone 1) size in each example is as indicated by FAA criteria for the approach type assumed. Adjustment may be necessary if the Approach type differs.
2. See Figure 3A for factors to consider regarding other possible adjustments to these zones to reflect characteristics of a specific airport runway.
3. See Figures 4B through 4G for guidance on compatibility criteria applicable with each zone.

These examples are intended to provide general guidance for establishment of airport safety compatibility zones. They do not represent California Department of Transportation standards or policy.

FIGURE 3A CONTINUED

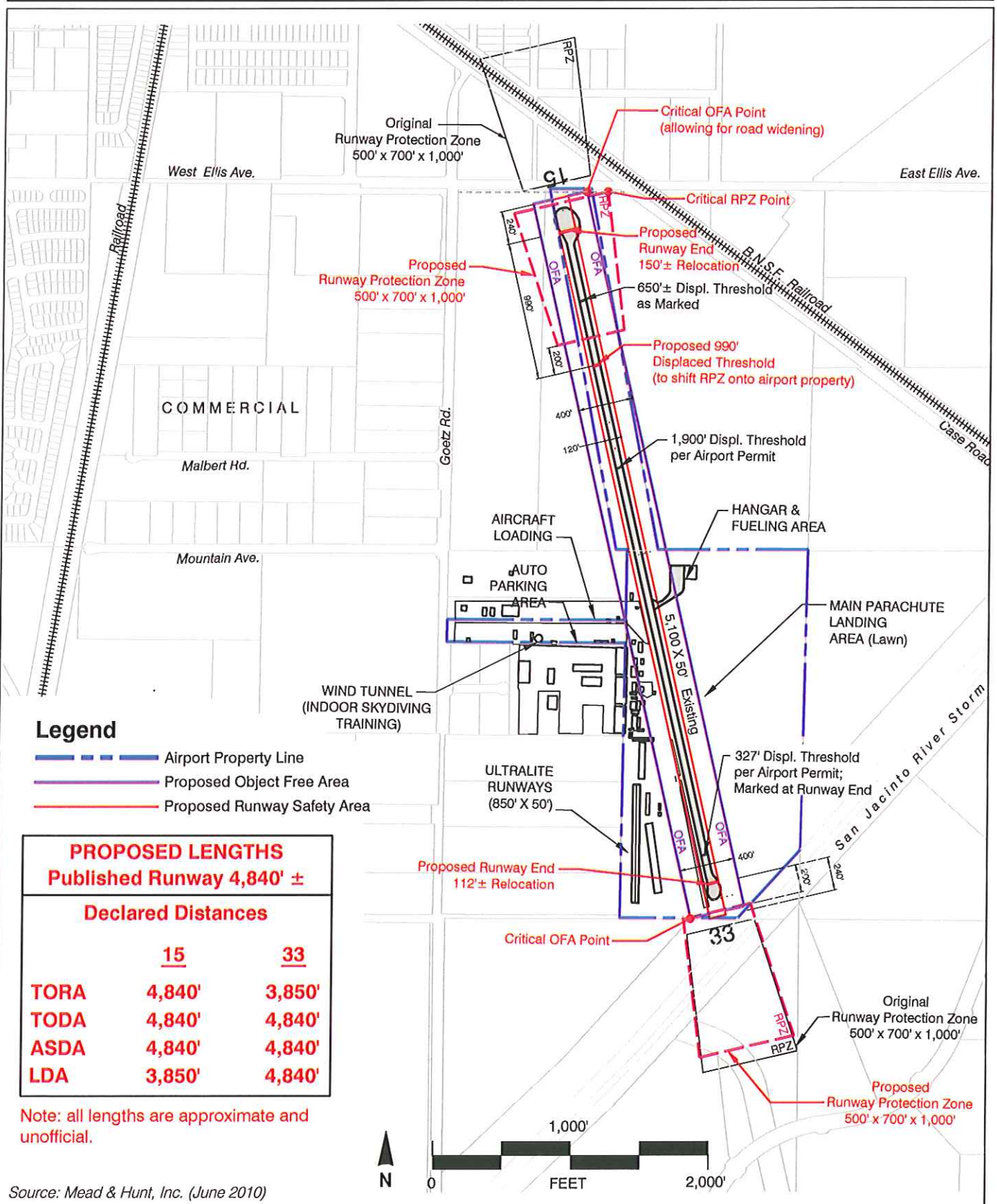
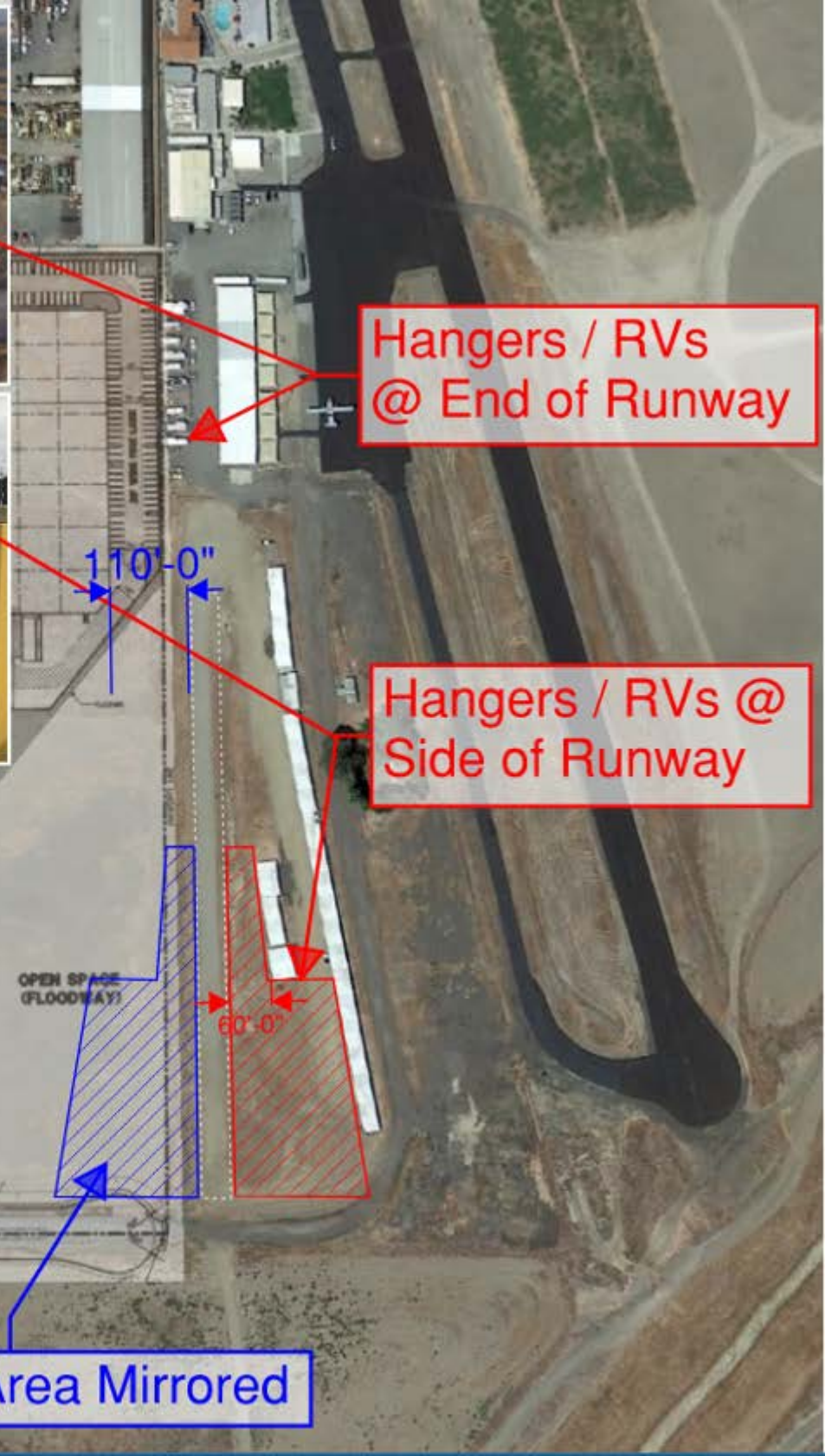


Exhibit PV-2

Airport Diagram

Perris Valley Airport

W8-4



Hangers / RVs @ End of Runway

Hangers / RVs @ Side of Runway

110'-0"

60'-0"

Clearance Area Mirrored



PARACHUTE DROP ZONE
AVOID!

800' RUNWAY

MAXIMUM 500' AGL

FIELD ELEVATION 1400' ASL
123.45 MHz

ARRIVING AIRCRAFT

MAXIMUM 200' AGL

DEPARTING AIRCRAFT

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

**THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS**

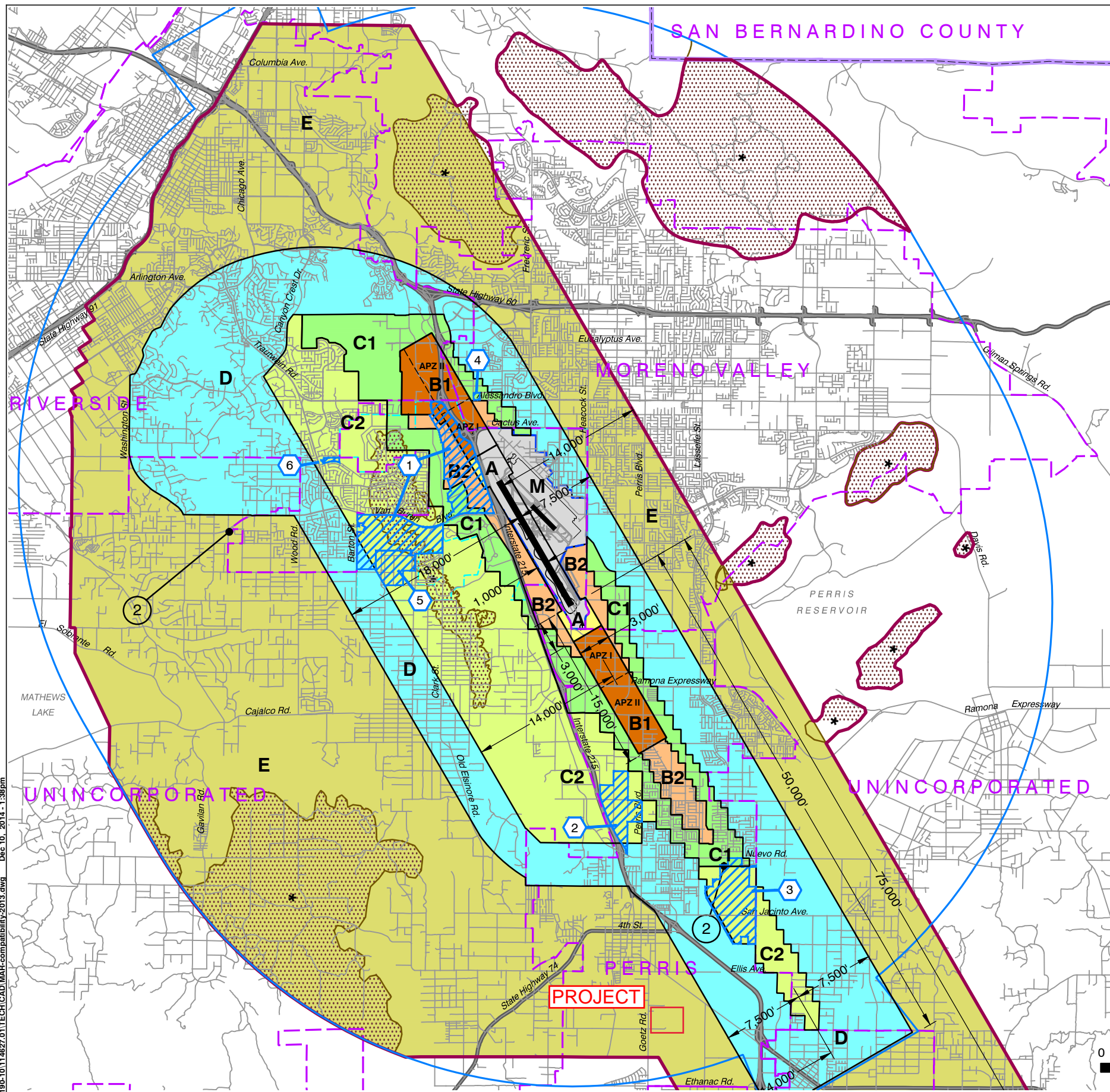
**PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES**



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



LEGEND

Compatibility Zones

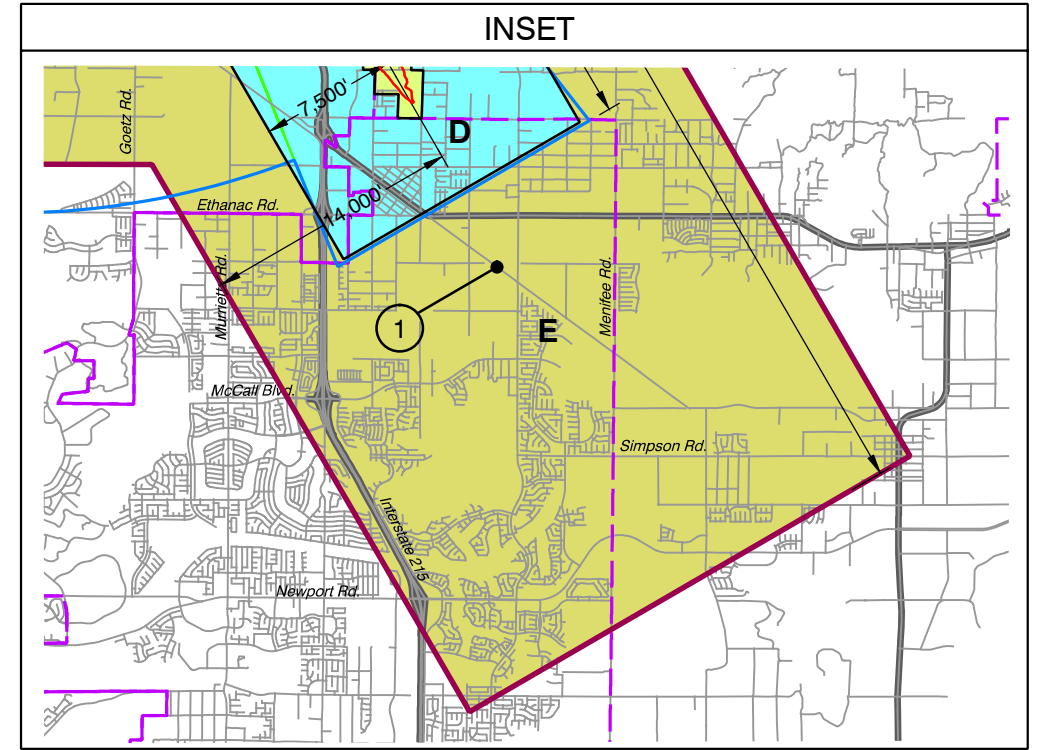
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

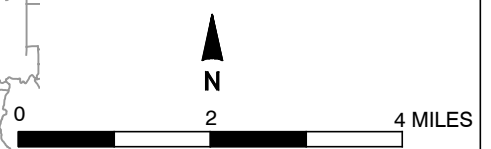
- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



**Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)**

Note:
All dimensions are measured from runway ends and centerlines.



Base map source: County of Riverside 2013

X:\18190-10\114627\01\TECH\CAD\MAR-compatibility\2013.dwg Dec 10, 2014 - 1:38pm

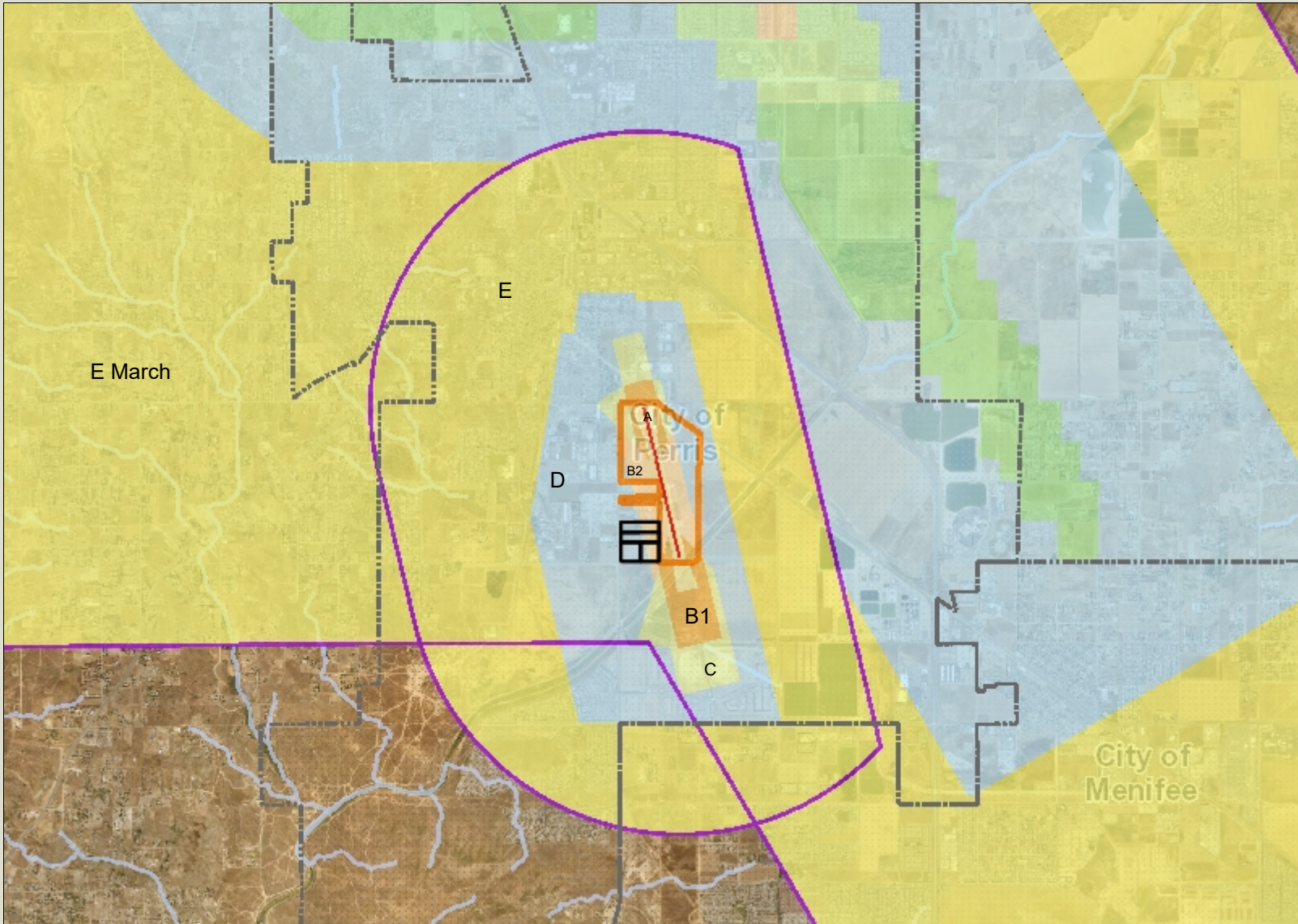
Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map MA-1

**Compatibility Map
March Air Reserve Base / Inland Port Airport**

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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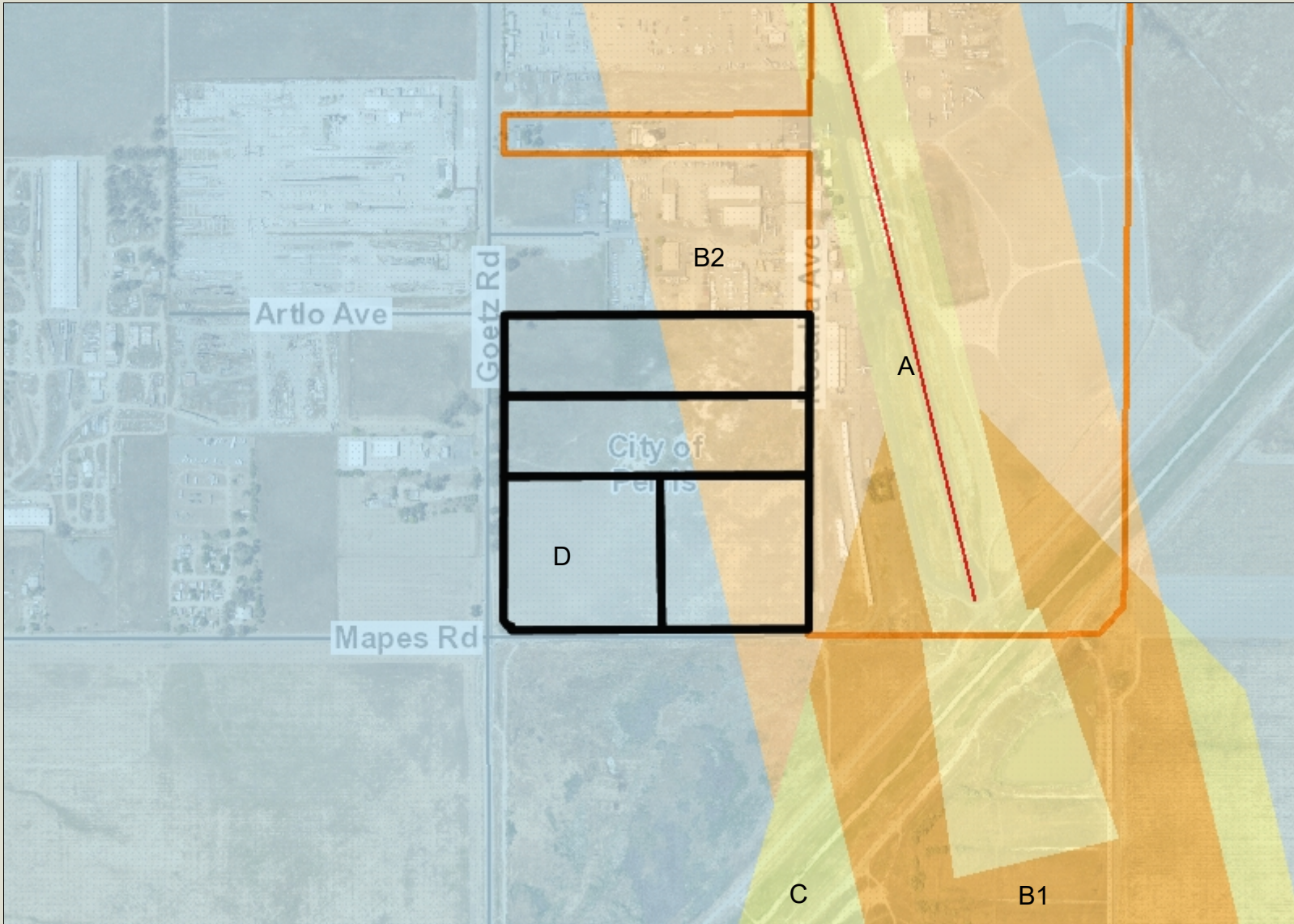


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Notes

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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Notes

Map My County Map



Legend

- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



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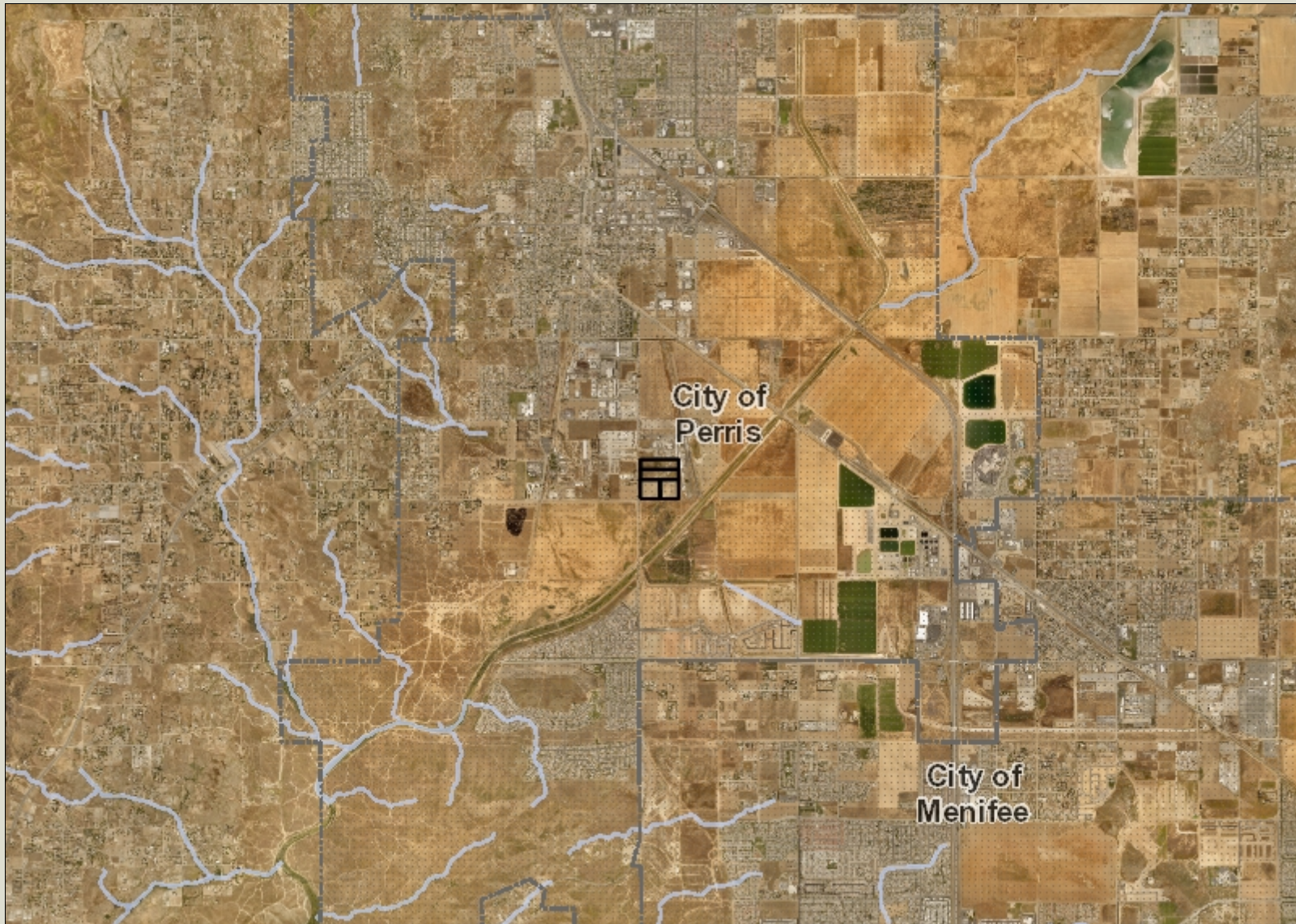
0 770 1,539 Feet

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Notes

Map My County Map



Legend

-  Blueline Streams
-  City Areas
-  World Street Map

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Notes



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Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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Notes



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Map My County Map



Legend

- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



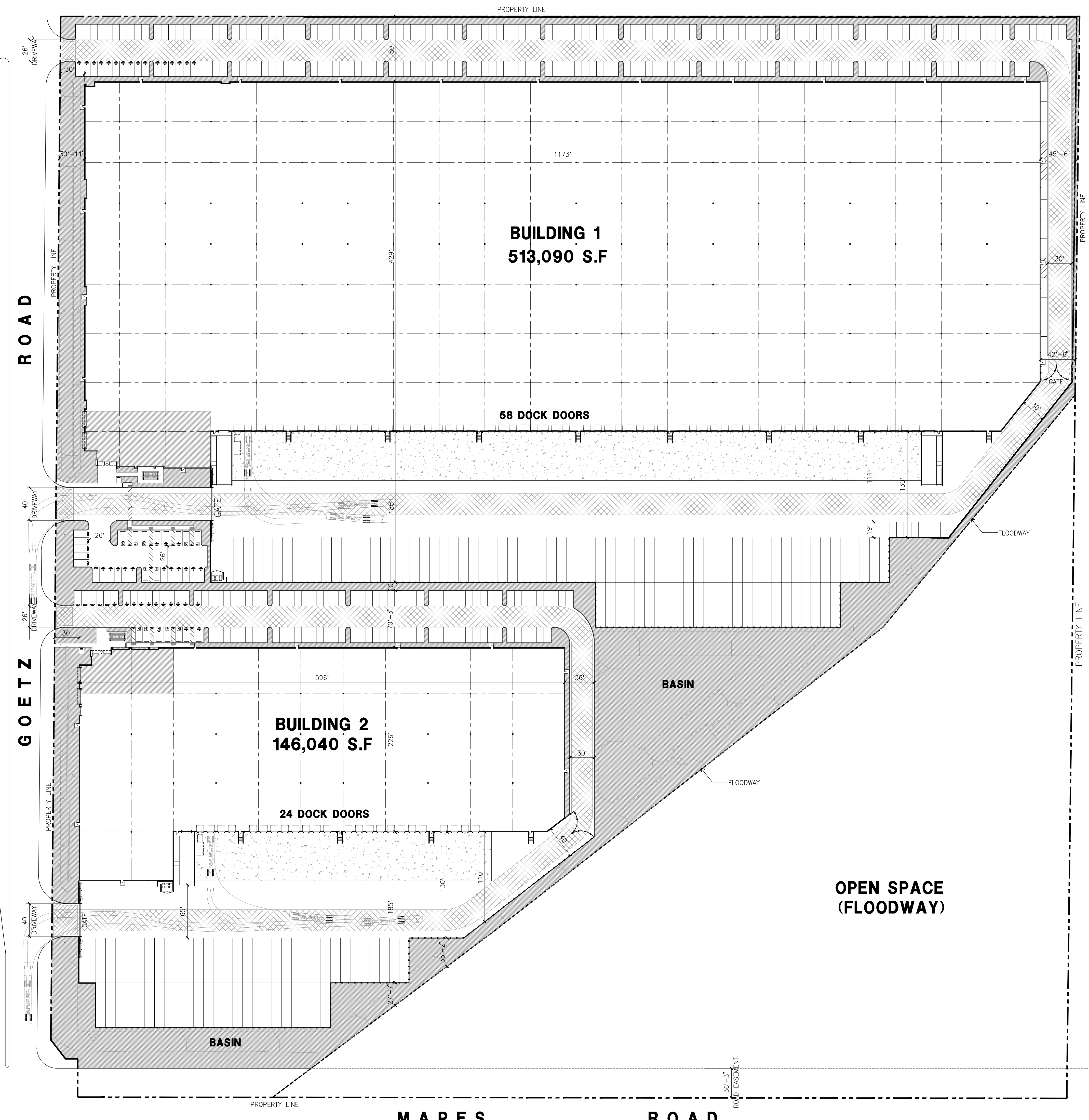
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

Notes



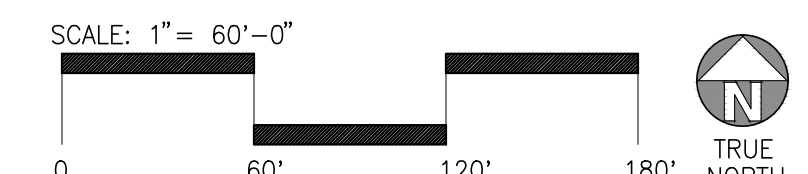
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MAPES ROAD

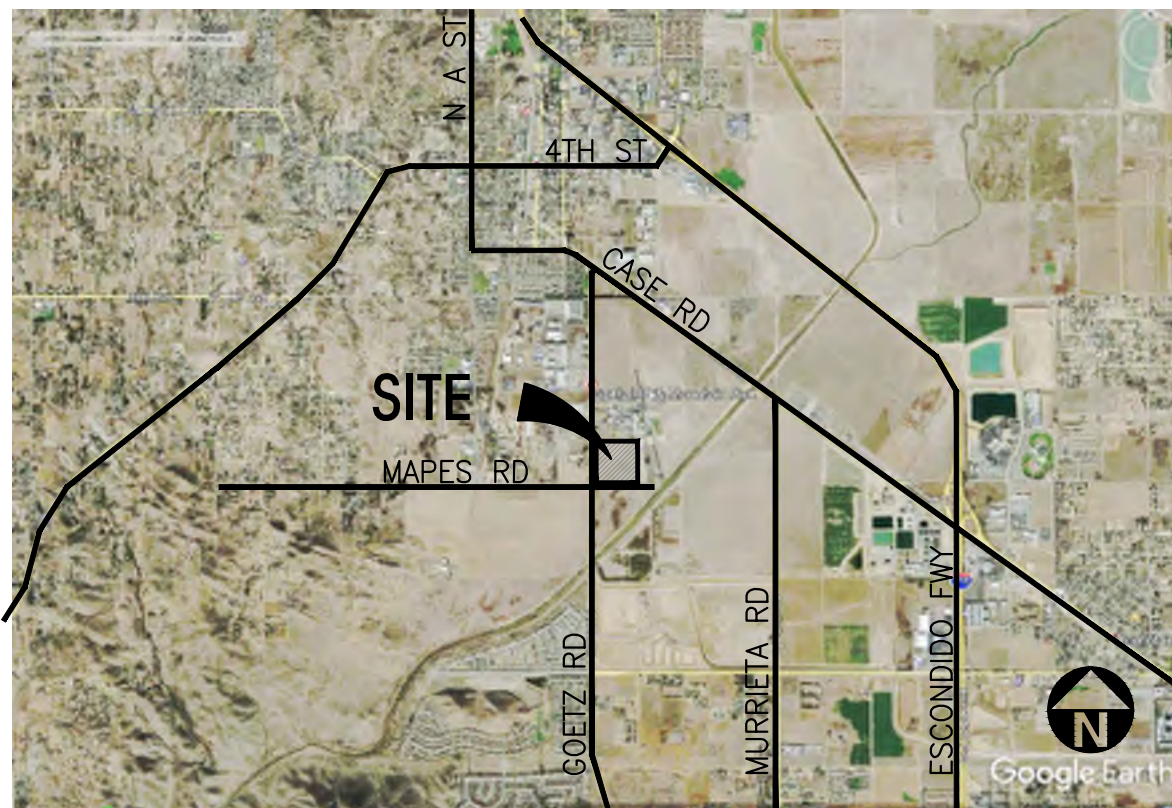
MASTER SITE PLAN
scale: 1" = 60'-0"



SITE LEGEND

- LANDSCAPED AREA
- AC. PAVING - SEE "C" DRWS. FOR THICKNESS
- CONCRETE PAVING SEE "C" DRWS. FOR THICKNESS
- CHAIN LINK FENCE
- PROPERTY LINE
- STANDARD PARKING STALL (9' X 19')
- HANDICAP PARKING STALL (9' X 19')
- PATH OF TRAVEL
- LIGHT STANDARD
- EXISTING PUBLIC FIRE HYDRANT
- PRIVATE FIRE HYDRANT - APPROXIMATE LOCATION
- PEDESTRIAN WALKWAY WITH LED WARNING LIGHTS
- CATCH BASIN, APPROX. LOCATION. SEE CIVIL DRWS.
- WATER LINE SEE CIVIL DRWS.
- GAS LINE SEE CIVIL DRWS.
- SEWER LATERAL SEE CIVIL DRWS.

VICINITY MAP



PROJECT DATA

SITE AREA	BLDG.1	BLDG.2	TOTAL
In s.f.			1,635,480 s.f.
In acres			37.55 ac
BUILDING AREA			
Office 1st Floor	5,000	5,000	10,000 s.f.
Office 2nd Floor	5,000	5,000	10,000 s.f.
Warehouse	503,090	136,040	639,130 s.f.
TOTAL	513,090	146,040	659,130 s.f.
FAR			40.3%
AUTO PARKING REQUIRED			
Office: 1/300 s.f.	33	33	67 stalls
Whse: 1st 20K @ 1/1,000 s.f.	20	20	40 stalls
above 20K @ 1/2,000 s.f.	242	58	300 stalls
TOTAL	295	111	406 stalls
AUTO PARKING PROVIDED			
Standard (9' x 19')	252	92	
Accessible Parking (9' x 19')	5	4	
Accessible Van Parking (12' x 19')	2	1	
EV Parking (9' x 19')	27	11	
EV Standard Accessible (9' x 19')	1	1	
EV Van Accessible (12' x 19')	1	1	
EV Ambulatory (9' x 19')	1	0	
Clean Air/ Van pool (9' x 19')	6	5	
Total	295	115	410 stalls
TRAILER PARKING PROVIDED			
Trailer (12' x 55')	92	57	149 stalls
ZONING ORDINANCE FOR CITY			
Zoning Designation - Light Industrial			
MAXIMUM BUILDING HEIGHT ALLOWED			
Height - 50'			
MAXIMUM FLOOR AREA RATIO			
FAR - 0.75			
SETBACKS			
Mapes Rd. - 10'			Note: Front yards for structures > 20' in height shall be increased by 5' for each 10' of structure height
Goetz Rd. - 15'			
Rear - None			

GENERAL NOTES - SITE PLAN

- A. THE SOILS REPORT PREPARED BY SOCIAL GEO PROJ. NO. _____ DATED ON _____ SHOULD BE A PART OF THESE CONTRACT DOCUMENTS.
- B. IF SOILS ARE EXPANSIVE IN NATURE, USE STEEL REINFORCING FOR ALL SITE CONCRETE.
- C. ALL DIMENSIONS ARE TO THE FACE OF CONCRETE WALL, FACE OF CONCRETE CURB OR GRID LINE UNL.O.
- D. SEE "C" PLANS FOR ALL CONCRETE CURBS, GUTTERS AND SWALES. DETAILS ON SHEET AD.1 ARE MINIMUM STANDARDS.
- E. THE ENTIRE PROJECT SHALL BE PERMANENTLY MAINTAINED WITH AN AUTOMATIC IRRIGATION SYSTEM. PRIOR TO INSTALLATION & AT LEAST 60 DAYS BEFORE BLDG. COMPLETION.
- F. SEE "C" DRAWINGS FOR POINT OF CONNECTIONS TO OFF-SITE UTILITIES. CONTRACTOR SHALL VERIFY ACTUAL UTILITY CONTRACTOR SHALL VERIFY ACTUAL UTILITY LOCATIONS.
- G. PROVIDE POSITIVE DRAINAGE AWAY FROM BLDG. SEE "C" DRAWINGS.
- H. CONTRACTOR TO REFER TO "C" DRAWINGS FOR ALL HORIZONTAL CONTROL DIMENSIONS. SITE PLANS ARE FOR GUIDANCE AND STARTING LAYOUT POINTS.
- I. SEE "C" DRAWINGS FOR FINISH GRADE ELEVATIONS.
- J. CONCRETE SIDEWALKS TO BE A MINIMUM OF 4" THICK W/ TOOLED JOINTS AT 6' O.C. EXPANSION/CONSTRUCTION JOINTS SHALL BE A MAXIMUM 12' EA. WAY W/ 1:20 MAX. SLOPE. EXPANSION JOINTS TO HAVE COMPRESSIVE EXPANSION FILLER MATERIAL OF 1/4". SEE "C" DRAWINGS FOR FINISH.
- K. PAINT CURBS AND PROVIDE SIGNS TO INFORM OF FIRE LANES AS REQUIRED BY FIRE DEPARTMENT.
- L. CONSTRUCTION DOCUMENTS PERTAINING TO THE LANDSCAPE AND IRRIGATION OF THE ENTIRE PROJECT SITE SHALL BE SUBMITTED TO THE BUILDING DEPARTMENT AND APPROVED BY PUBLIC FACILITIES DEVELOPMENT PRIOR TO ISSUANCE OF BUILDING PERMITS.
- M. PRIOR TO FINAL CITY INSPECTION, THE LANDSCAPE ARCHITECT SHALL SUBMIT A CERTIFICATE OF COMPLETION TO PUBLIC FACILITIES DEVELOPMENT.
- N. ALL LANDSCAPE AND IRRIGATION DESIGNS SHALL MEET CURRENT CITY STANDARDS AS LISTED IN GUIDELINES OR AS OBTAINED FROM PUBLIC FACILITIES DEVELOPMENT.
- O. LANDSCAPED AREAS SHALL BE DELINEATED WITH A MINIMUM SIX INCHES (6") HIGH CURB.
- P. APPROVED CONCEPTUAL LANDSCAPE PLAN PRIOR TO GRADING PERMIT
- Q. WITHIN ZONES C-1 AND D, RESTRICTION APPLY AND CONDITIONS OF APPROVAL WILL BE APPLIED TO ADDRESS DEVELOPMENT IN THESE ZONES.

PROPERTY OWNER

RICHLAND
3161 MICHELSON DRIVE, SUITE 425, IRVINE, CA 92615
CONTACT: DEREK BARBOUR
TEL: 714.932.0650
EMAIL: DBARBOR@RICHLANDCOMMUNITIES.COM

ADDRESS OF THE PROPERTY

MAPES RD. AND GOETZ RD.
CITY OF PERRIS

ASSESSOR'S PARCEL NUMBER

330-100-005 330-100-027
330-100-006 330-100-025

LEGAL DESCRIPTION

PARCEL ONE: (APN: 330-100-005)

LOT 5 OF EHRMAN'S SUBDIVISION OF THE WEST HALF OF THE SOUTHWEST QUARTER OF SECTION 5 IN TOWNSHIP 5 SOUTH, RANGE 3 WEST, SAN BERNARDINO BASE AND MERIDIAN, AS SHOWN BY MAP RECORDED IN BOOK 11 PAGE 544 OF MAPS, SAN DIEGO COUNTY RECORDS, EXCEPTING THEREFROM THAT PORTION OF LAND CONVEYED TO THE CITY OF PERRIS, A MUNICIPAL CORPORATION AND GENERAL LAW CITY, IN GRANT DEED RECORDED JUNE 28, 2019, AS INSTRUMENT NO. 2019-0236246 OF OFFICIAL RECORDS.

PARCEL TWO: (APN: 330-100-006)

LOT 6 OF EHRMAN'S SUBDIVISION OF THE WEST HALF OF THE SOUTHWEST QUARTER OF SECTION 5 IN TOWNSHIP 5 SOUTH, RANGE 3 WEST, SAN BERNARDINO BASE AND MERIDIAN, AS SHOWN BY MAP RECORDED IN BOOK 11 PAGE 544 OF MAPS, SAN DIEGO COUNTY RECORDS, EXCEPTING THEREFROM THAT PORTION OF LAND CONVEYED TO THE CITY OF PERRIS, A MUNICIPAL CORPORATION AND GENERAL LAW CITY, IN GRANT DEED RECORDED JUNE 28, 2019, AS INSTRUMENT NO. 2019-0250218, BOTH OF OFFICIAL RECORDS.

PARCEL A: (APN: 330-100-027)

PARCEL AS SHOWN ON LOT LINE ADJUSTMENT NO. 05-0352, AS EVIDENCED BY DOCUMENT RECORDED NOVEMBER 14, 2005 AS INSTRUMENT NO. 2005-943442 OF OFFICIAL RECORDS, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

THOSE PORTIONS OF LOTS 7 AND 8 OF EHRMAN'S SUBDIVISION OF THE WEST HALF OF THE SOUTHWEST QUARTER OF SECTION 5 IN TOWNSHIP 5 SOUTH, RANGE 3 WEST, SAN BERNARDINO BASE AND MERIDIAN, IN THE CITY OF PERRIS, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 11, PAGE 544, OF MAPS, SAN DIEGO COUNTY RECORDER, DESCRIBE AS FOLLOWS:

BEGINNING AT THE SOUTHWEST CORNER OF SAID LOT 8, BEING 30.00 FEET EAST OF THE CENTERLINE OF GOETZ ROAD; THENCE NORTH 89° 44' 45" EAST, ALONG THE SOUTH LINE OF SAID LOT 8, A DISTANCE OF 671.86 FEET; THENCE NORTH 0° 15' 15" WEST, A DISTANCE OF 662.97 FEET TO A POINT ON THE NORTH LINE OF SAID LOT 7; THENCE SOUTH 89° 42' 51" WEST, ALONG THE NORTH LINE OF SAID LOT 7, BEING 30.00 FEET EAST OF THE CENTERLINE OF GOETZ ROAD; THENCE SOUTH 0° 08' 58" WEST, ALONG SAID GOETZ ROAD, A DISTANCE OF 662.62 FEET TO THE POINT OF BEGINNING; EXCEPTING THEREFROM THAT PORTION OF LAND CONVEYED TO RECORDED ON JUNE 14, 2019 AS INSTRUMENT NO. 2019-0215610 OF OFFICIAL RECORDS.

PARCEL B: (APN: 330-100-025)

PARCEL 2 AS SHOWN ON LOT LINE ADJUSTMENT NO. 05-0352, AS EVIDENCED BY DOCUMENT RECORDED NOVEMBER 14, 2005 AS INSTRUMENT NO. 2005-943442 OF OFFICIAL RECORDS, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

THOSE PORTIONS OF LOTS 7 AND 8 OF EHRMAN'S SUBDIVISION OF THE WEST HALF OF THE SOUTHWEST QUARTER OF SECTION 5 IN TOWNSHIP 5 SOUTH, RANGE 3 WEST, SAN BERNARDINO BASE AND MERIDIAN, IN THE CITY OF PERRIS, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 11, PAGE 544 OF MAPS, SAN DIEGO COUNTY RECORDER, DESCRIBED AS FOLLOWS:

COMMENCING AT THE SOUTHWEST CORNER OF SAID LOT 8, BEING 30.00 FEET EAST OF THE CENTERLINE OF GOETZ ROAD; THENCE NORTH 89° 44' 45" EAST, ALONG THE SOUTH LINE OF SAID LOT 8, A DISTANCE OF 671.86 FEET TO THE POINT OF BEGINNING; THENCE NORTH 0° 15' 15" WEST, A DISTANCE OF 662.97 FEET TO A POINT IN THE NORTH LINE OF SAID LOT 7; THENCE NORTH 89° 42' 51" EAST, ALONG THE NORTH LINE OF SAID LOT 7, A DISTANCE OF 616.17 FEET TO THE NORTHEAST CORNER OF SAID LOT 7; THENCE SOUTH 0° 21' 16" WEST, ALONG THE EAST LINES OF SAID LOTS 7 AND 8, A DISTANCE OF 663.35 FEET TO THE SOUTHEAST CORNER OF SAID LOT 8; THENCE SOUTH 89° 44' 45" WEST, ALONG THE SOUTH LINE OF SAID LOT 8, A DISTANCE OF 609.12 FEET TO THE POINT OF BEGINNING.

ZONING

ZONING DESIGNATION - LIGHT INDUSTRIAL

APPLICANT

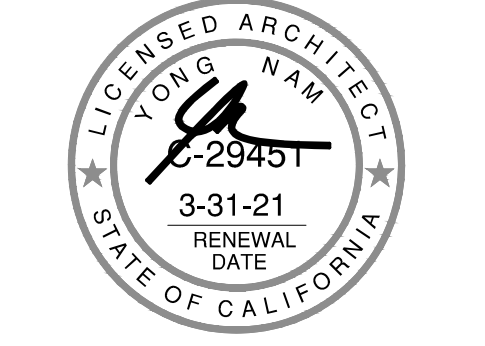
HPA, INC.
18831 BARDEEN AVE SUITE 100
IRVINE CA 92612
TEL: (949) 862-2127
CONTACT: YUNXUAN GUO

APPLICANT'S REPRESENTATIVE

HPA, INC.
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Owner:



RICHLAND
3161 Michelson Drive, Suite 425,
Irvine, CA 92615
tel: 714.932.0650

Project:

MAPES RD AND
GOETZ RD.

PERRIS, CA

Consultants:

- CIVIL - SB&O
- STRUCTURAL -
- MECHANICAL -
- PLUMBING -
- ELECTRICAL -
- LANDSCAPE - EMERALD DESIGN
- FIRE PROTECTION -
- SOILS ENGINEER -

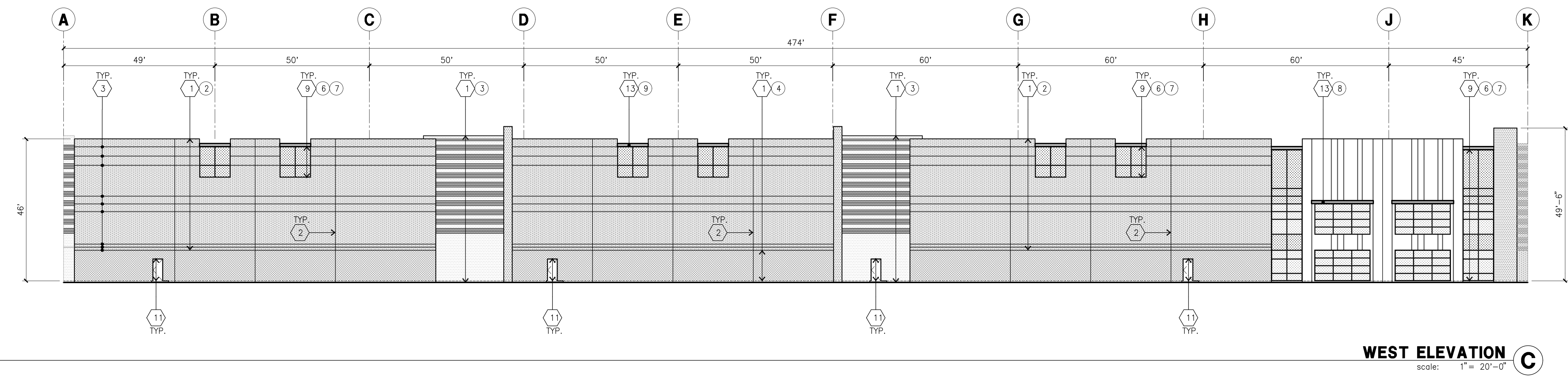
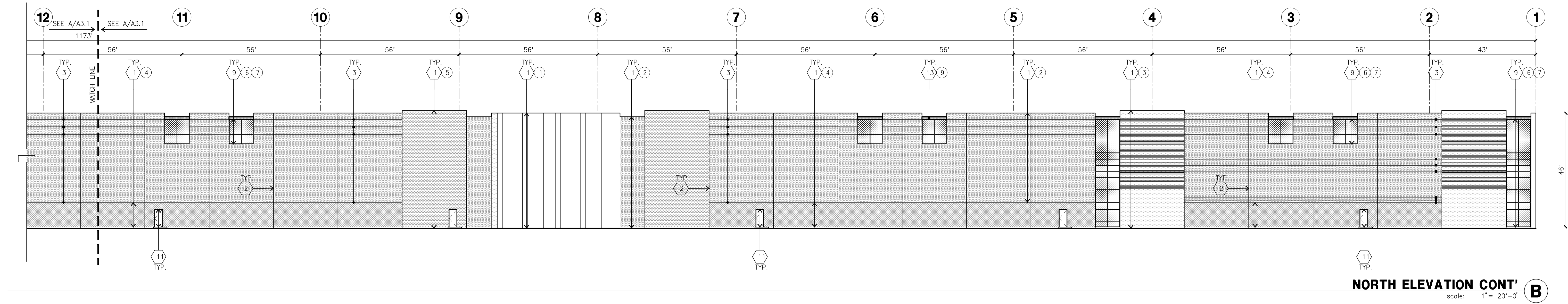
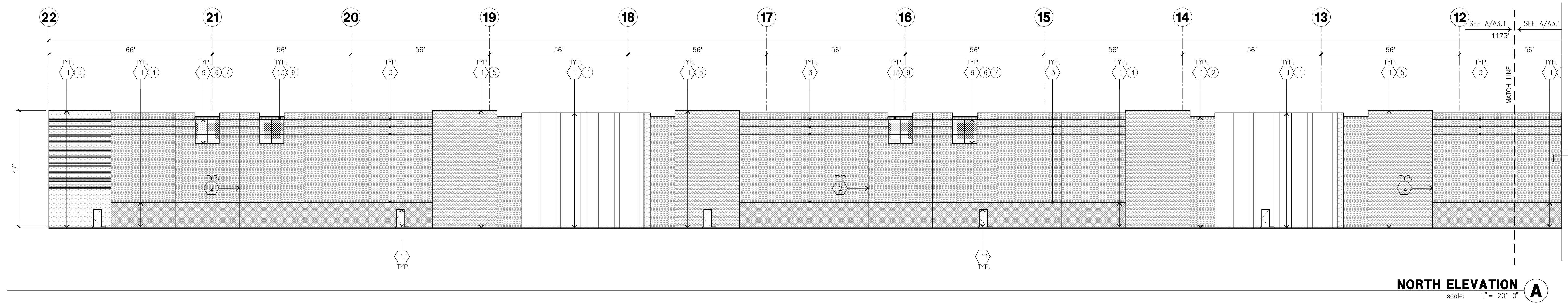
Title: MASTER SITE PLAN

Project Number: 20219
Drawn by: Y.G.
Date: 02/04/22
Revision:

Sheet:

DAB-A1.0
PAM16-160

OFFICIAL USE ONLY



KEYNOTES - ELEVATIONS

- 1 CONCRETE TILT-UP PANEL (PAINTED).
- 2 PANEL JOINT.
- 3 PANEL REVEAL.
- 4 CONCRETE TILT-UP SCREEN WALL.
- 5 OVERHEAD INSULATED DOOR @ DOCK HIGH.
- 6 OVERHEAD DOOR @ DRIVE THRU.
- 7 CONCRETE STAIR, LANDING AND CONC. GUARDRAIL.
- 8 DOCK BUMPER.
- 9 ALUMINUM STOREFRONT FRAMING W/ TEMPERED GLAZING AT ALL DOORS SIDELITES ADJACENT TO DOORS AND GLAZING W/ BOTTOMS LESS THAN 18" ABOVE F.F. ELEVATION.
- 10 LOUVER (SIZE AND LOCATION APPROX. ONLY).
- 11 HOLLOW METAL DOORS.
- 12 INTERIOR ROOF DRAIN WITH OVERFLOW SCUPPER.
- 13 EXTERIOR CANOPY.
- 14 CORONADO STONE VENEER.

COLOR SCHEDULE - ELEVATIONS

- | | | | | |
|---|------------------------|------------|---|---------------------------------------|
| 1 | CONCRETE TILT-UP PANEL | PAIN | BRAND | SHERWIN WILLIAMS SW 7000 PURE WHITE |
| 2 | CONCRETE TILT-UP PANEL | PAIN | BRAND | SHERWIN WILLIAMS SW 7071 GRAY SCREEN |
| 3 | CONCRETE TILT-UP PANEL | PAIN | BRAND | SHERWIN WILLIAMS SW 7073 NETWORK GRAY |
| 4 | CONCRETE TILT-UP PANEL | PAIN | BRAND | SHERWIN WILLIAMS SW 7074 SOFTWARE |
| 5 | CONCRETE TILT-UP PANEL | PAIN | BRAND | SHERWIN WILLIAMS SW 7076 CYBERSPACE |
| 6 | MULLIONS | COLOR | CLEAR ANODIZED | |
| 7 | GLAZING | FINISH | BLUE REFLECTIVE | |
| 8 | METAL CANOPY | MODEL NAME | SHERWIN WILLIAMS ACRYLIC LATEX SYSTEMS HIGH GLOSS/HIGH PERFORMANCE SW 7072 ONLINE | |
| 9 | METAL CANOPY | MODEL NAME | SHERWIN WILLIAMS ACRYLIC LATEX SYSTEMS HIGH GLOSS/HIGH PERFORMANCE SW 7074 SOFTWARE | |

GLAZING LEGEND

- TEMPERED VISION GLASS
 - TEMPERED SPANDELL GLASS
- ALL GLASS TO BE NON-REFLECTIVE

GENERAL NOTES - ELEVATIONS

- A. ALL PAINT COLOR CHANGES TO OCCUR AT INSIDE CORNERS UNLESS NOTED OTHERWISE.
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- F. CONTRACTOR SHALL FULLY PAINT ONE CONCRETE PANEL W/ SELECTED COLORS. ARCHITECT AND OWNER SHALL APPROVE PRIOR TO PAINTING REMAINDER OF BUILDING.



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Project:
MAPES RD AND
GOETZ RD.
PERRIS, CA

Consultants:

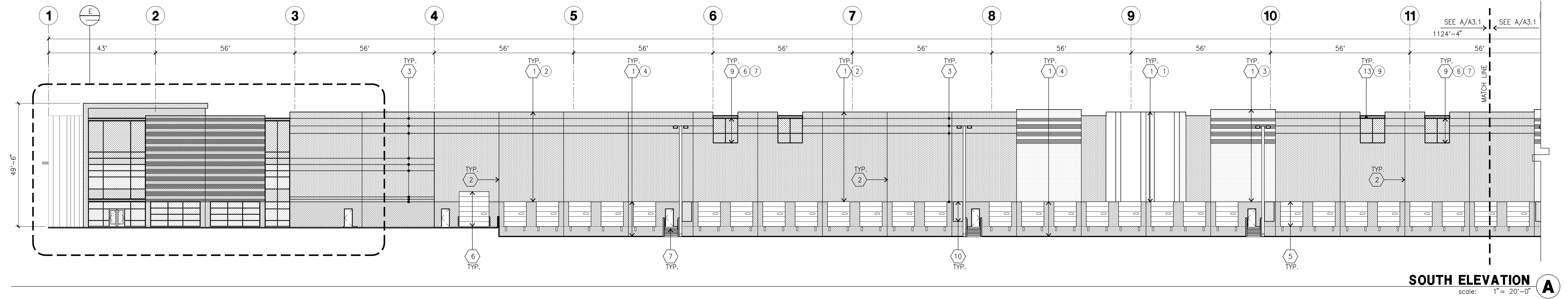
CIVIL	SB&O
STRUCTURAL	-
MECHANICAL	-
PLUMBING	-
ELECTRICAL	-
LANDSCAPE	EMERALD DESIGN
FIRE PROTECTION	-
SOILS ENGINEER	-

Title: ELEVATIONS

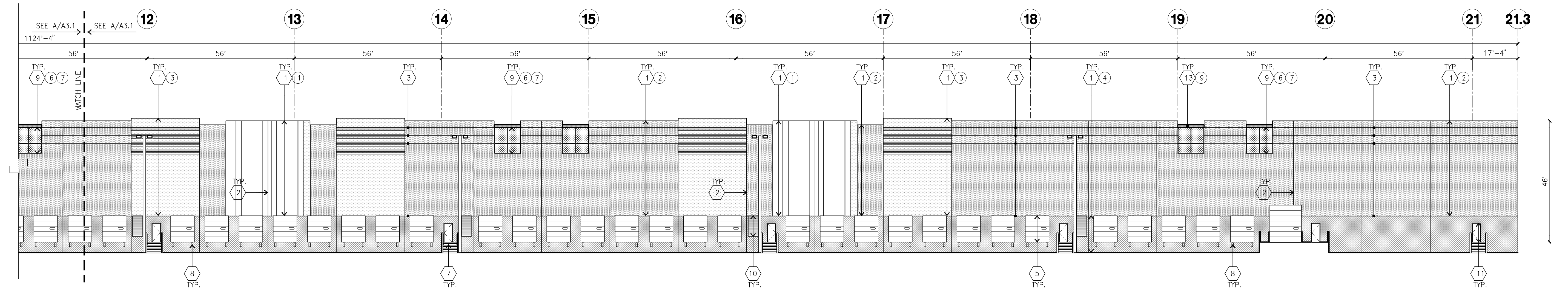
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Drawn by: Y.G.
Date: 02/04/22
Revision:

Sheet:

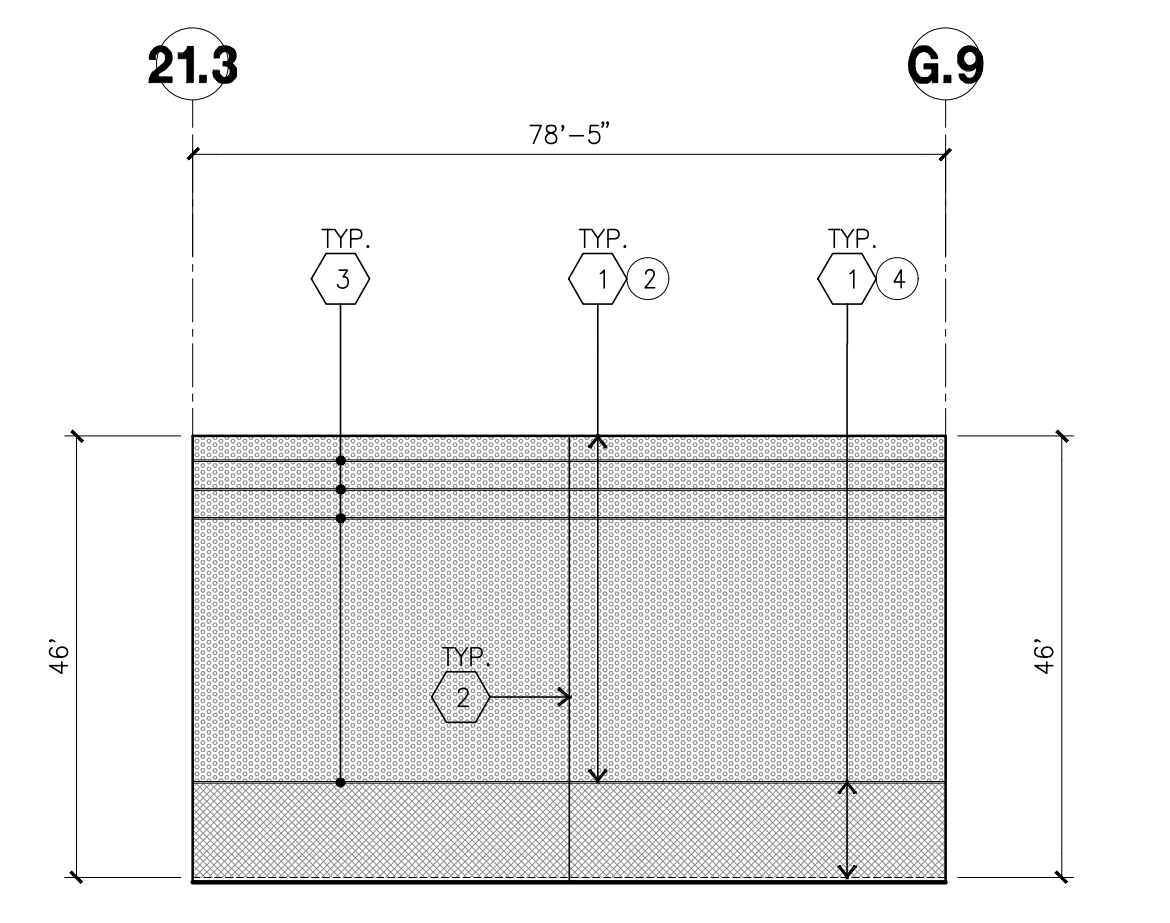
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PAM16-160



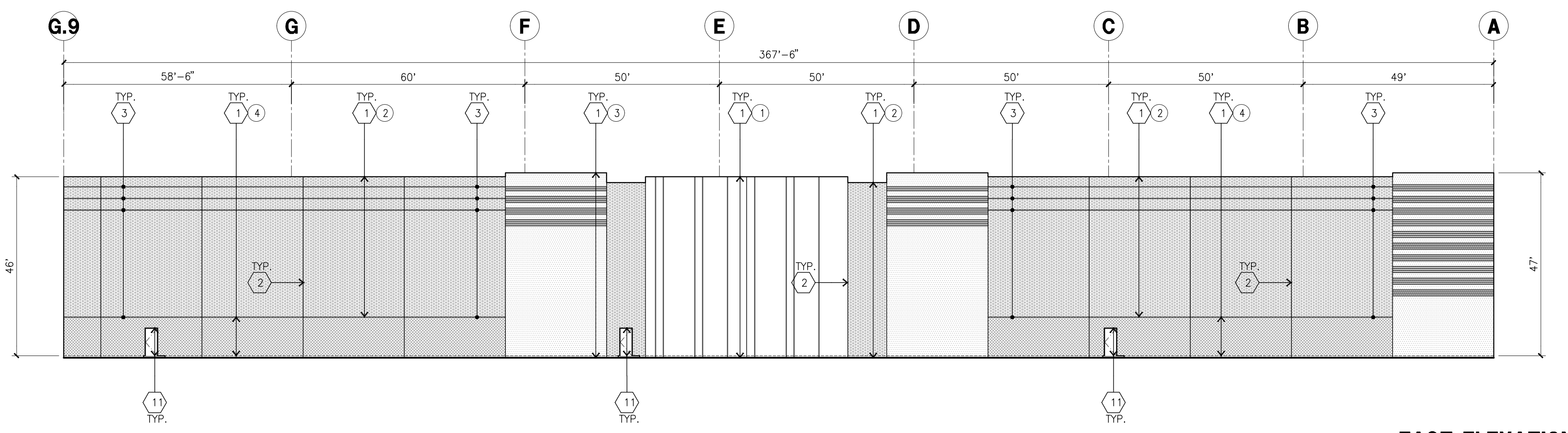
SOUTH ELEVATION A
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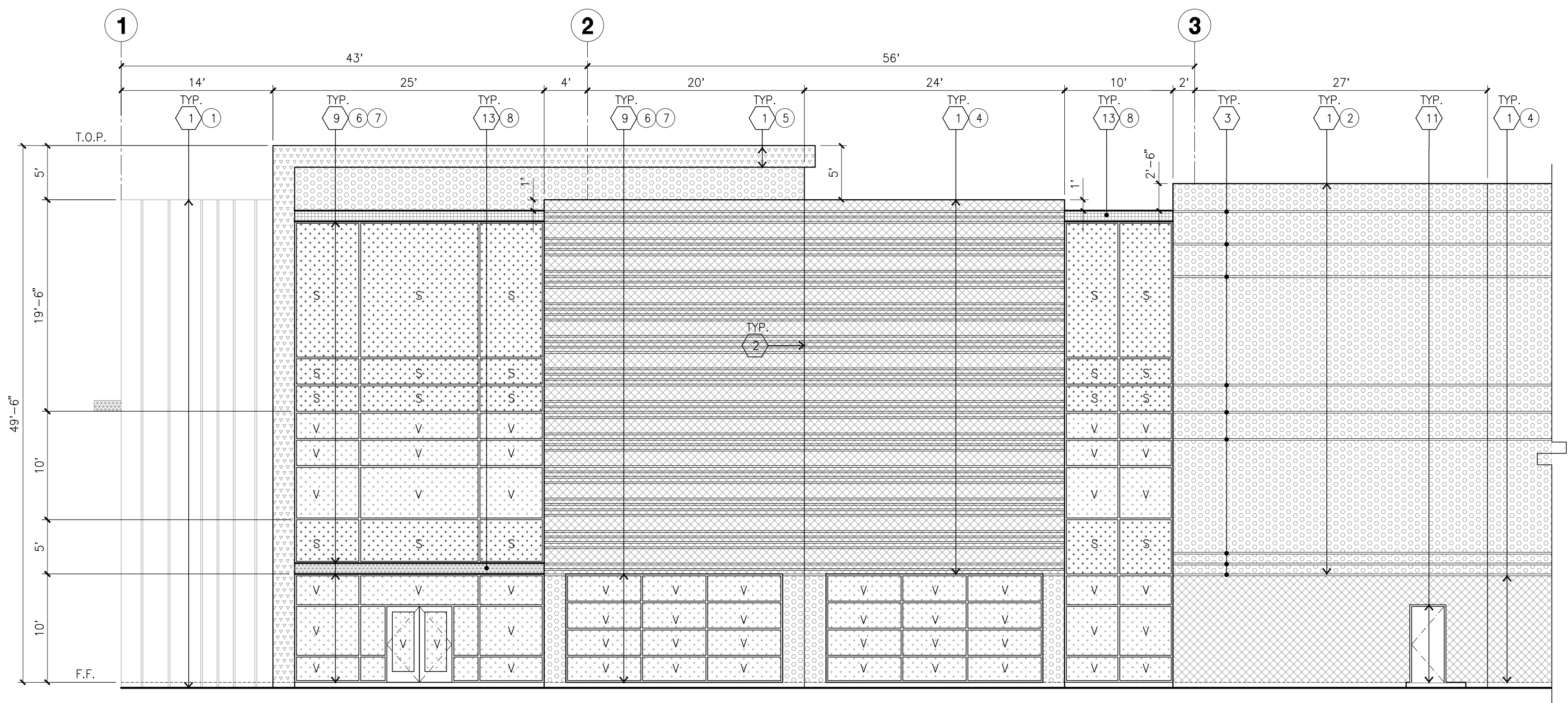
SOUTH ELEVATION CONT' B
scale: 1" = 20'-0"



NORTH ELEVATION D
scale: 1" = 20'-0"



EAST ELEVATION C
scale: 1" = 20'-0"



ENLARGED SOUTH ELEVATION E
scale: 1/8" = 1'-0"

KEYNOTES - ELEVATIONS

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GLAZING LEGEND

- V TEMPERED VISION GLASS
 - S TEMPERED SPANDREL GLASS
- ALL GLASS TO BE NON-REFLECTIVE.

COLOR SCHEDULE - ELEVATIONS

1	CONCRETE TILT-UP PANEL	PAIN BRAND	SHERWIN WILLIAMS SW 7005 PURE WHITE
2	CONCRETE TILT-UP PANEL	PAIN BRAND	SHERWIN WILLIAMS SW 7071 GRAY SCREEN
3	CONCRETE TILT-UP PANEL	PAIN BRAND	SHERWIN WILLIAMS SW 7073 NETWORK GRAY
4	CONCRETE TILT-UP PANEL	PAIN BRAND	SHERWIN WILLIAMS SW 7074 SOFTWARE
5	CONCRETE TILT-UP PANEL	PAIN BRAND	SHERWIN WILLIAMS SW 7076 CYBERSPACE
6	MULLIONS	COLOR	CLEAR ANODIZED
7	GLAZING	FINISH	BLUE REFLECTIVE
8	METAL CANOPY	MODEL NAME	SHERWIN WILLIAMS ACRYLIC LATEX SYSTEMS HIGH GLOSS/HIGH PERFORMANCE SW 7072 ONLINE
9	METAL CANOPY	MODEL NAME	SHERWIN WILLIAMS ACRYLIC LATEX SYSTEMS HIGH GLOSS/HIGH PERFORMANCE SW 7074 SOFTWARE

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- F. CONTRACTOR SHALL FULLY PAINT ONE CONCRETE PANEL W/ SELECTED COLORS. ARCHITECT AND OWNER SHALL APPROVE PRIOR TO PAINTING REMAINDER OF BUILDING.

HPA architecture

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LICENSED ARCHITECT
YOUNG NAM
9-23-15
9-31-21
RENEWAL DATE
STATE OF CALIFORNIA

Owner:

RICHLAND COMPUNITIES

RICHLAND

3161 Michelson Drive, Suite 425,
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tel: 714.932.0650

Project:

MAPES RD AND GOETZ RD.

PERRIS, CA

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CIVIL	SB&O
STRUCTURAL	-
MECHANICAL	-
PLUMBING	-
ELECTRICAL	-
LANDSCAPE	EMERALD DESIGN
FIRE PROTECTION	-
SOILS ENGINEER	-

Title: ELEVATIONS

Project Number: 20219

Drawn by: Y.G

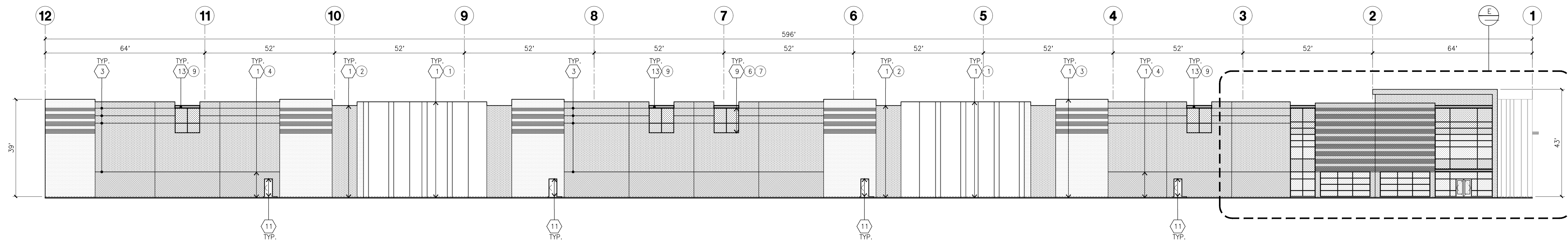
Date: 02/04/22

Revision:

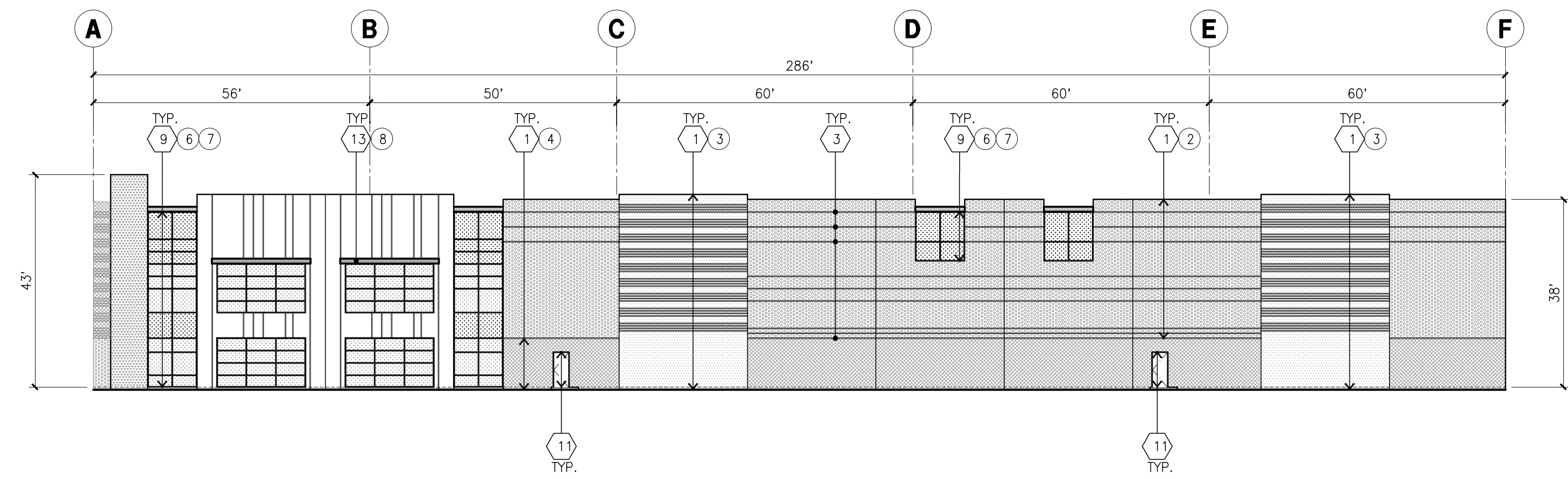
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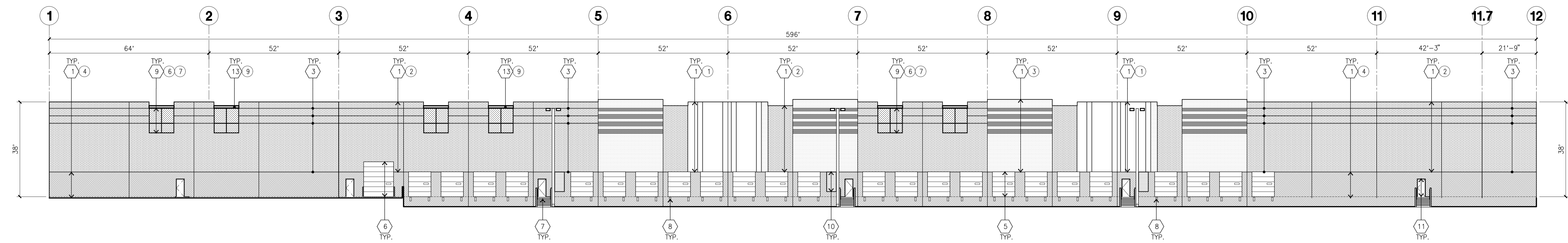
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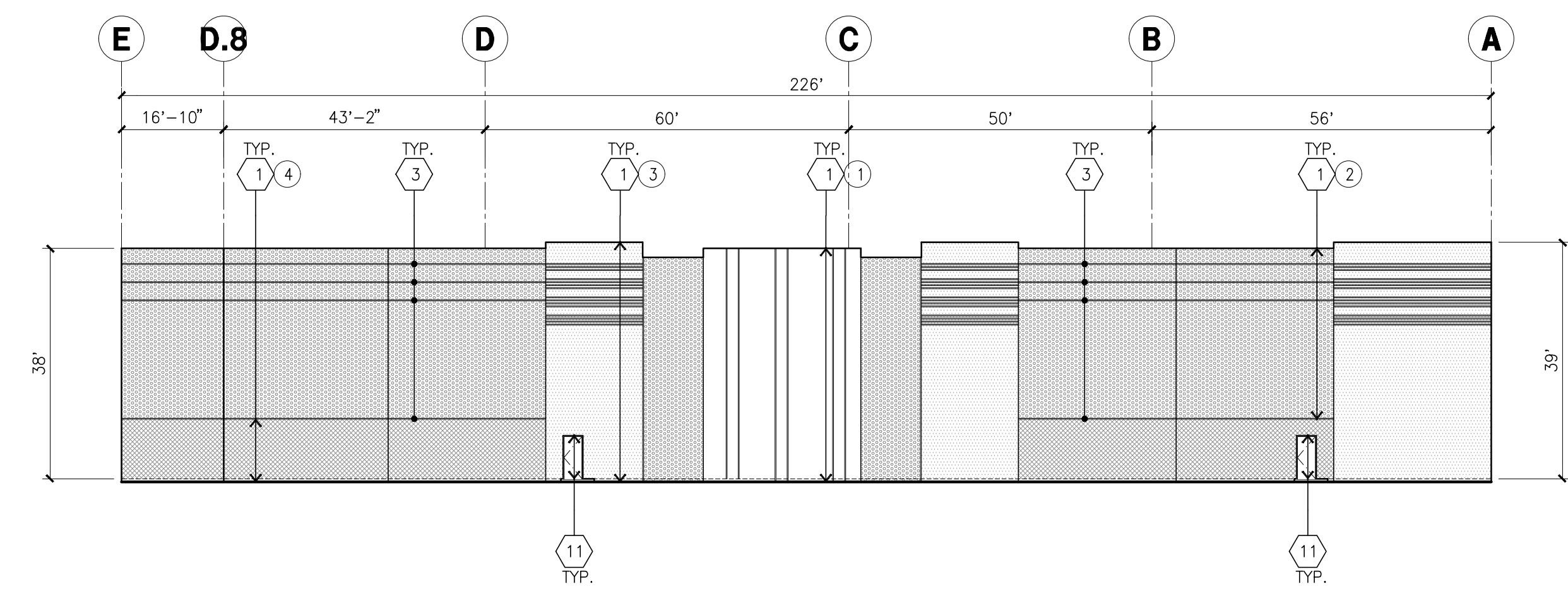
NORTH ELEVATION
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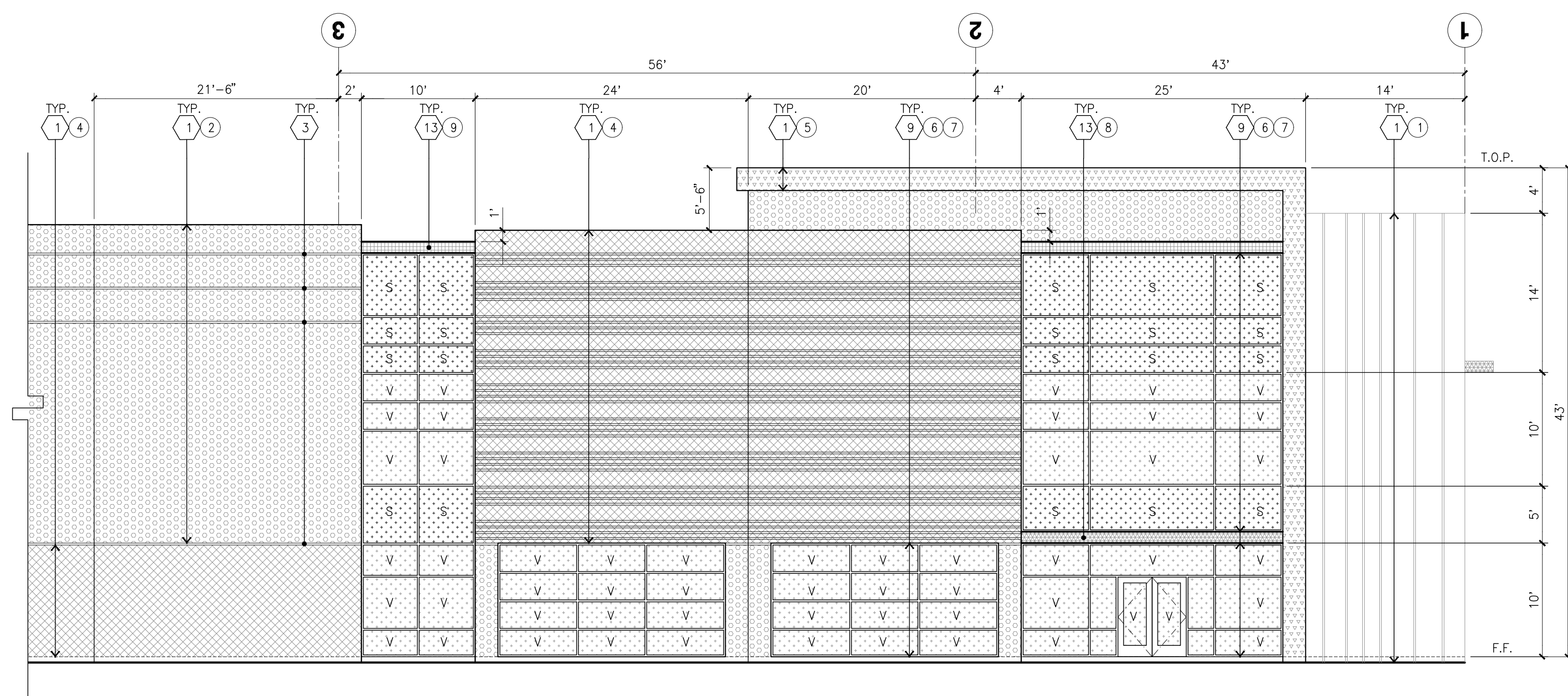
WEST ELEVATION
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SOUTH ELEVATION
scale: 1" = 20'-0"



EAST ELEVATION
scale: 1" = 20'-0"



ENLARGED NORTH ELEVATION
scale: 1/8" = 1'-0"

KEYNOTES - ELEVATIONS

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COLOR SCHEDULE - ELEVATIONS

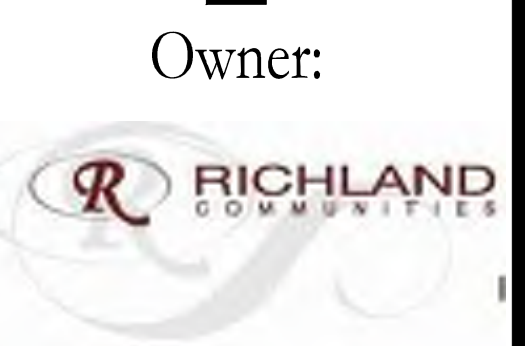
- | | | | |
|--|--------------------------|-------------|--|
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| | 7 GLAZING | FINISH | BLUE REFLECTIVE |
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LANDSCAPE	EMERALD DESIGN
FIRE PROTECTION	-
SOILS ENGINEER	-

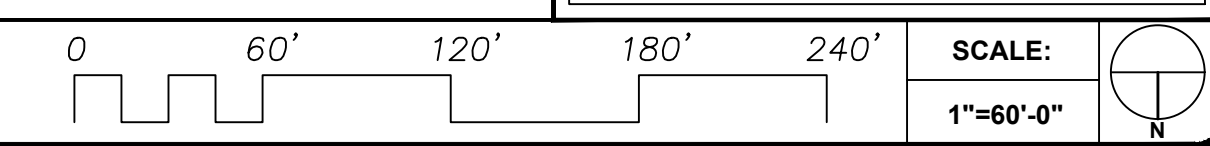
Title: ELEVATIONS

Project Number: 20219
Drawn by: Y.G
Date: 02/04/22
Revision:

Sheet:
2-DAB-A3.1
PAM16-160



PROGRESS DRAWING
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DATE	REVISION	BY

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 30 N. Harbor Blvd, Suite 232
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 Email: charles@emeraldhd.com

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RICHLAND PERRIS
 CORNER OF MAPES RD & GOETZ RD
 CITY OF PERRIS

LANDSCAPE CONCEPT

JOB NAME: RICHLAND PERRIS
 DRAWN BY: EFIAC
 CHECKED BY: CL
 DATE: 2/2/22

SHEET NO.
LC1

SHRUB / GROUND COVER LEGEND

SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE	WATER USE
1 SHRUB PER 30 SF OF LANDSCAPE, 80% TO BE 5 GALLON OR LARGER				
PARKING LOT SCREENING SHRUBS				
	WESTRINGIA 'BLUE GEM'	BLUE GEM COAST ROSEMARY	5 GAL	LOW
	CALLISTEMON 'LITTLE JOHN'	DWARF BOTTLE BRUSH	5 GAL	LOW
SLOPE AND/OR BIOSWALE PLANTING				
	BACCHARIS 'PIGEON POINT'	DWARF COYOTE BUSH		
	ROSMARINUS 'HUNTINGTON CARPET'	HUNTINGTON CARPET ROSEMARY		
	SALVIA APIANA	WHITE SAGE		
	WESTRINGIA 'BLUE GEM'	BLUE GEM WESTRINGIA		
ENHANCED PLANTING				
	AGAVE PARRYI VAR. TRUNCATA	ARTICHOKE AGAVE	5 GAL	LOW
	BULBINE FRUTESCENS	BULBINE	1 GAL	LOW
	CHAMAEROPS HUMILIS MULTI TRUNK	MEDITERRANEAN FAN PALM	15 GAL	LOW
	DIANELLA 'CLARITY BLUE'	CLARITY BLUE DIANELLA	1 GAL	LOW
	HESPERALOE PARVIFLORA	RED YUCCA	5 GAL	VERY LOW
	SENECIO SERPENS	BLUE CHALKSTICKS	1 GAL	LOW
TYPICAL PLANTING				
	AGAVE AMERICANA	CENTURY PLANT		VERY LOW
	CAESALPINIA GILLIESII	DESERT BIRD OF PARADISE	5 GAL	LOW
	CALLISTEMON 'LITTLE JOHN'	DWARF BOTTLEBRUSH	5 GAL	LOW
	DASYLIRION WHEELERI	DESERT SPOON	5 GAL	LOW
	GALVEZIA SPECIOSA 'FIRECRACKER'	ISLAND BUSH SNAPDRAGON	5 GAL	LOW
	MUHLENBERGIA RIGENS	DEER GRASS	5 GAL	LOW
	RHAMNUS CALIFORNICA 'EVE CASE'	COFFEEBERRY 'EVE CASE'	5 GAL	LOW
	ROSMARINUS 'HUNTINGTON CARPET'	HUNTINGTON CARPET ROSEMARY	1 GAL	LOW
	TRICHOSTEMA LANATUM	WOOLLY BLUE CURLS	5 GAL	LOW
	LARGE DECORATIVE BOULDERS			

TREE LEGEND

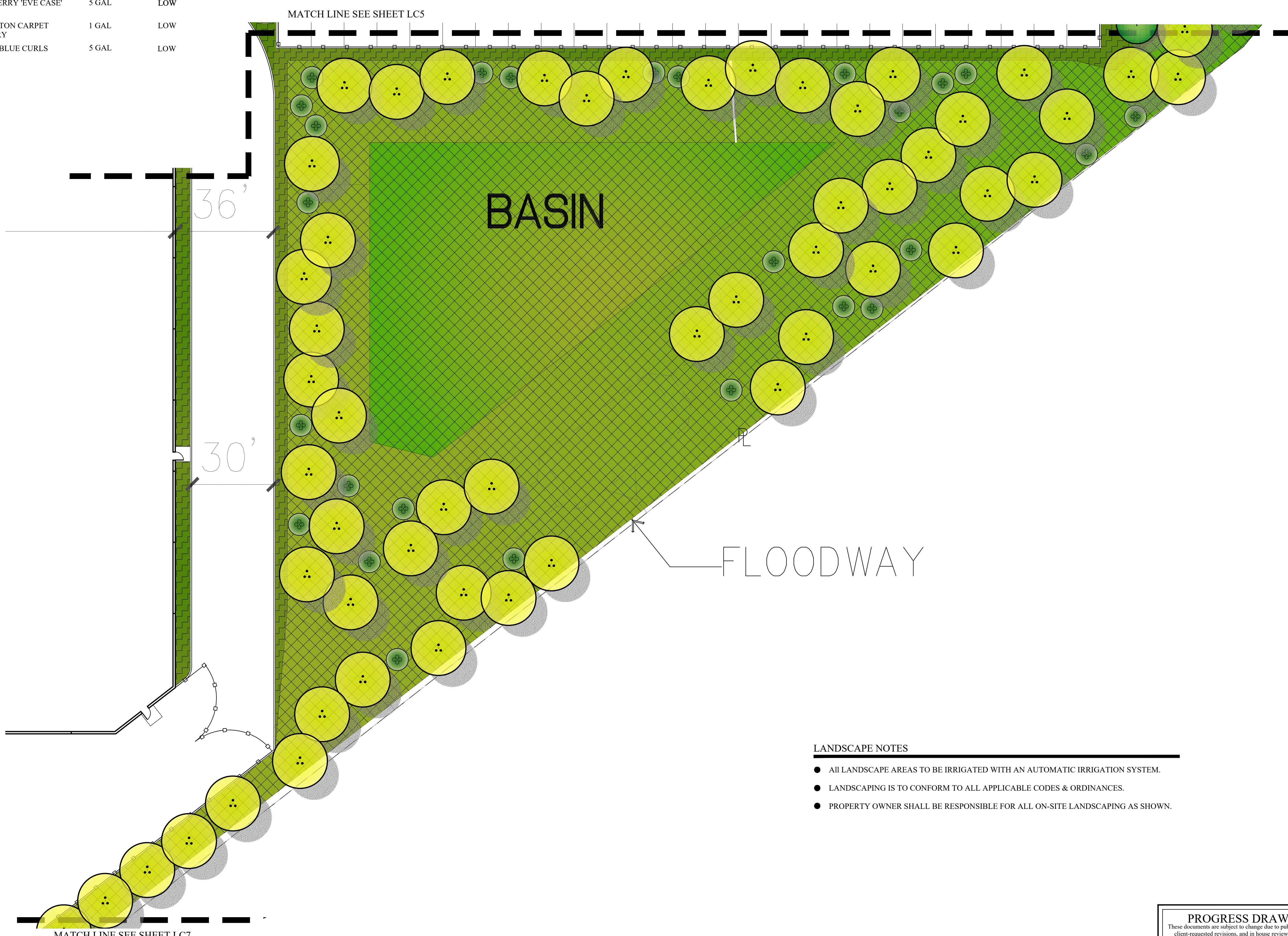
SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE	QUANTITY	WATER USE
	BRACHYCHITON POPULNEUS	BOTTLE TREE	24" BOX	149	LOW
	CERCIDIUM 'DESERT MUSEUM' MULTI TRUNK	DESERT MUSEUM PALO VERDE	36" BOX	16	LOW
	PINUS HALEPENSIS	ALEPPO PINE	24" BOX	24	LOW
	RHUS LANCEA STANDARD TRUNK	AFRICAN SUMAC	36" BOX	36	LOW
	RHUS LANCEA MULTI TRUNK	AFRICAN SUMAC	36" BOX	5	LOW

PARKING LOT TREE REQUIREMENTS:
 TOTAL PARKING LOT TREES REQUIRED = 67
 TOTAL PARKING LOT TREES PROVIDED = 138
 TOTAL TREES @ 36" BOX SIZE = 51 (37%)

SLOPE PLANT LEGEND (SLOPE = 55,580 SF)

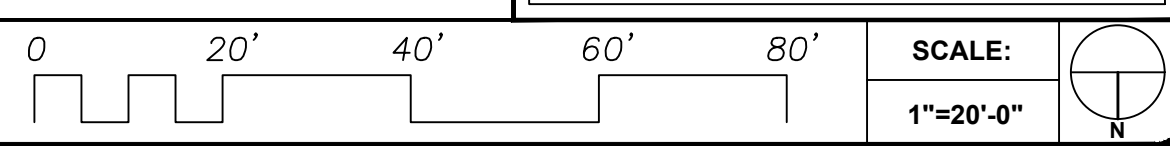
SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE	QUANTITY	WATER USE
SLOPE TREES REQUIRED = 139 SLOPE TREES PROVIDED = 15 GALLON- 97 (70%), 5 GALLON- 42 (30%)					
	CERCIDIUM 'DESERT MUSEUM' MULTI TRUNK	DESERT MUSEUM PALO VERDE	15 GAL	97	LOW
	HETEROMELES ARBUTIFOLIA	TOYON TREE	5 GAL	42	LOW

SLOPE SHRUB REQUIREMENTS:
 5 GALLON SHRUBS EVERY 75 SF + GROUND COVER
 1 GALLON SHRUBS EVERY 60 SF



- LANDSCAPE NOTES**
- ALL LANDSCAPE AREAS TO BE IRRIGATED WITH AN AUTOMATIC IRRIGATION SYSTEM.
 - LANDSCAPING IS TO CONFORM TO ALL APPLICABLE CODES & ORDINANCES.
 - PROPERTY OWNER SHALL BE RESPONSIBLE FOR ALL ON-SITE LANDSCAPING AS SHOWN.

PROGRESS DRAWING
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 Fullerton, California 92832
 Phone: (714) 771-1111
 California License #10968
 Email: chris@emeraldsg.com

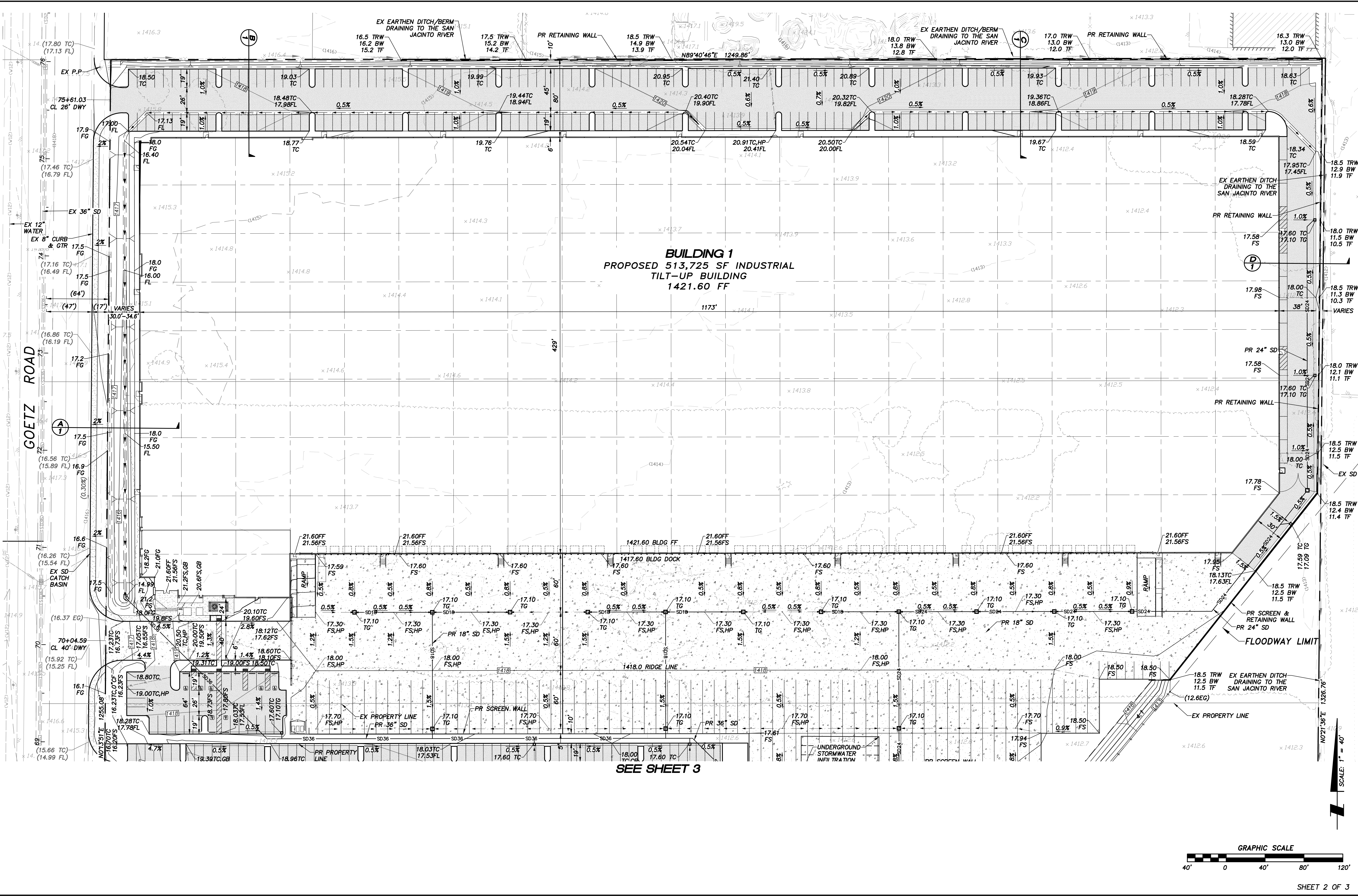
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 CITY OF PERRIS

LANDSCAPE CONCEPT

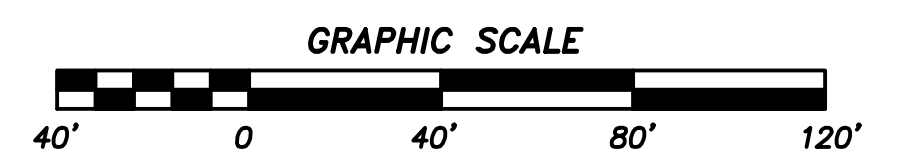
JOB NAME: RICHLAND PERRIS
 DRAWN BY: EFAC
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 DATE: 2/2/22

SHEET NO.
LC6

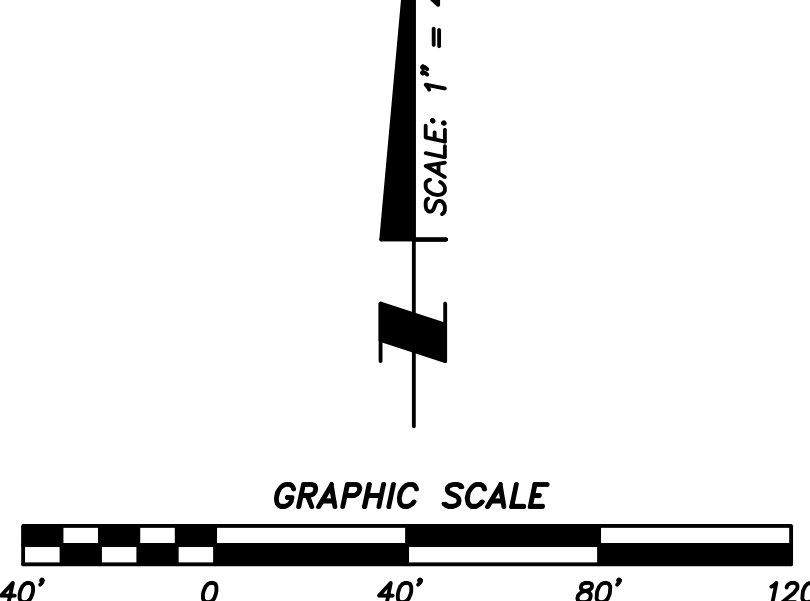
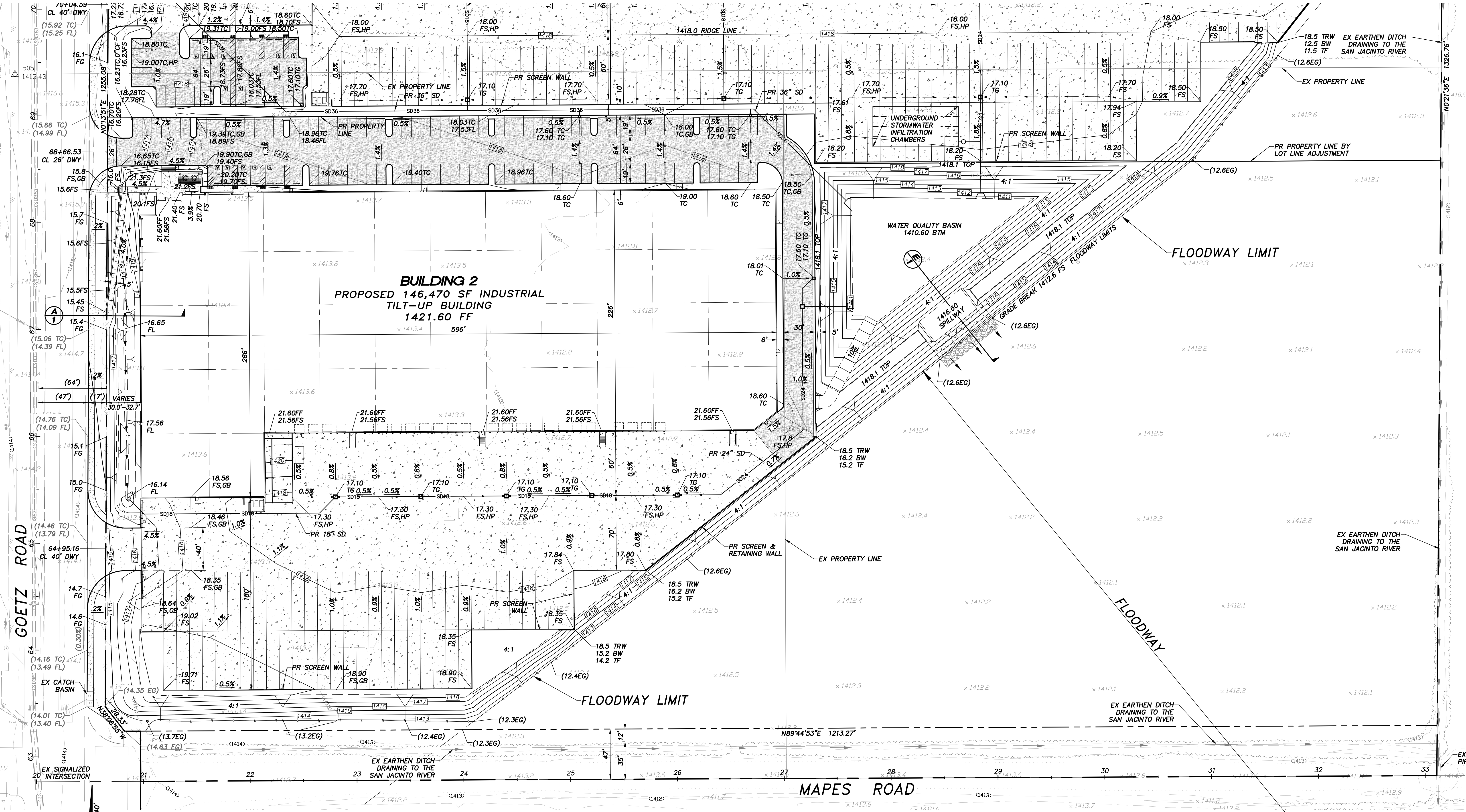


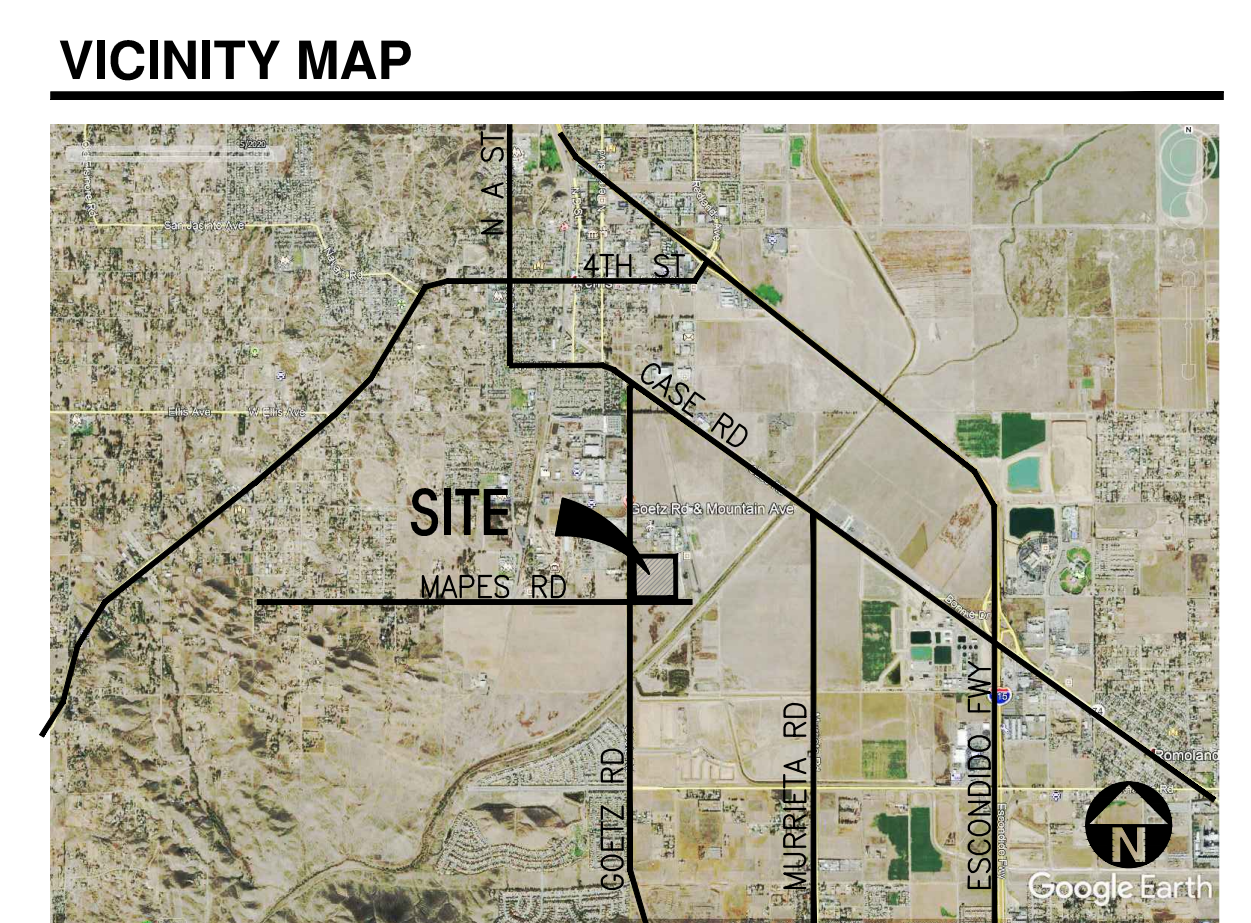
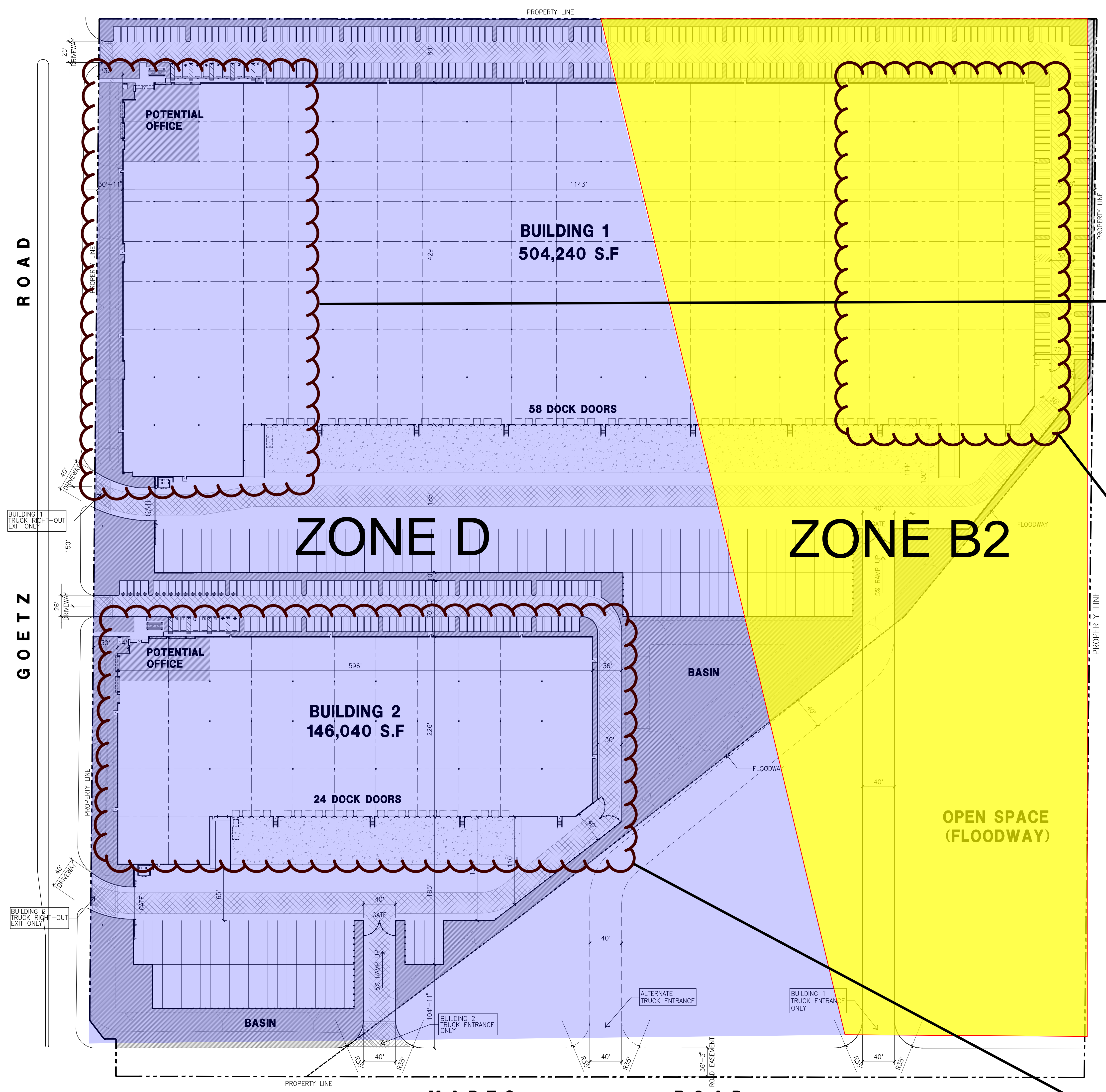
BUILDING 1
 PROPOSED 513,725 SF INDUSTRIAL
 TILT-UP BUILDING
 1421.60 FF

SEE SHEET 3



SEE SHEET 2





PROPERTY OWNER
 RICHLAND
 3161 MICHELSON DRIVE, SUITE 425, IRVINE, CA 92615
 CONTACT: DEREK BARBOUR
 TEL: 714.932.0650
 EMAIL: DBARBOR@RICHLANDCOMMUNITIES.COM

ADDRESS OF THE PROPERTY
 MAPES RD. AND GOETZ RD.

ASSESSOR'S PARCEL NUMBER
 330-100-005 330-100-027
 330-100-006 330-100-025

LEGAL DESCRIPTION
 PARCEL ONE: (APN: 330-100-005)
 LOT 5 OF EHRMAN'S SUBDIVISION OF THE WEST HALF OF THE SOUTHWEST QUARTER OF SECTION 5 IN TOWNSHIP 5 SOUTH, RANGE 3 WEST, SAN BERNARDINO BASE AND MERIDIAN, AS SHOWN BY MAP RECORDED IN BOOK 11 PAGE 544 OF MAPS, SAN DIEGO COUNTY RECORDS, EXCEPTING THEREFROM THAT PORTION OF LAND CONVEYED TO THE CITY OF PERRIS, A MUNICIPAL CORPORATION AND GENERAL LAW CITY, IN GRANT DEED RECORDED JUNE 28, 2019, AS INSTRUMENT NO. 2019-0236246 OF OFFICIAL RECORDS.

PROJECT DATA

Existing Grade: 1415.1
Proposed Grade: 1421.6
Proposed Building Height*: 46'
Proposed Roof Elevation: 1467.6
**Note: small sections of parapet walls to up to 47' tall*

Standard (9' x 19')	263	91
Accessible Parking (9' x 19')	6	4
Accessible Van Parking (12' x 19')	2	1
EV Parking (9' x 19')	28	11
EV Standard Accessible (9' x 19')	1	1

Existing Grade: 1412.5
Proposed Grade: 1421.6
Proposed Building Height*: 46'
Proposed Roof Elevation: 1467.6
**Note: small sections of parapet walls to up to 49.6' tall*

GENERAL NOTES - SITE PLAN

- THE SOILS REPORT PREPARED BY SOCAL GEO PROJ. NO. _____ DATED ON _____ SHOULD BE A PART OF THESE CONTRACT DOCUMENTS.
- IF SOILS ARE EXPANSIVE IN NATURE, USE STEEL REINFORCING FOR ALL SITE CONCRETE.
- ALL DIMENSIONS ARE TO THE FACE OF CONCRETE WALL, FACE OF CONCRETE CURB OR GRID LINE U.N.O.
- SEE "C" PLANS FOR ALL CONCRETE CURBS, GUTTERS AND SWALES. DETAILS ON SHEET AD.1 ARE MINIMUM STANDARDS.
- THE ENTIRE PROJECT SHALL BE PERMANENTLY MAINTAINED WITH AN AUTOMATIC IRRIGATION SYSTEM, PRIOR TO INSTALLATION & AT LEAST 60 DAYS BEFORE BLDG. COMPLETION.
- SEE "C" DRAWINGS FOR POINT OF CONNECTIONS TO OFF-SITE UTILITIES. CONTRACTOR SHALL VERIFY ACTUAL UTILITY CONTRACTOR SHALL VERIFY ACTUAL UTILITY LOCATIONS.
- PROVIDE POSITIVE DRAINAGE AWAY FROM BLDG. SEE "C" DRAWINGS.
- CONTRACTOR TO REFER TO "C" DRAWINGS FOR ALL HORIZONTAL CONTROL DIMENSIONS. SITE PLANS ARE FOR GUIDANCE AND STARTING LAYOUT POINTS.
- SEE "C" DRAWINGS FOR FINISH GRADE ELEVATIONS.
- CONCRETE SIDEWALKS TO BE A MINIMUM OF 4" THICK W/ TOOLED JOINTS AT 8' O.C. EXPANSION/CONSTRUCTION JOINTS SHALL BE A MAXIMUM 12' EA. WAY W/ 1:20 MAX. SLOPE. EXPANSION JOINTS TO HAVE COMPRESSIVE EXPANSION FILLER MATERIAL OF 1/4". SEE "L" DRAWINGS FOR FINISH.
- PAINT CURBS AND PROVIDE SIGNS TO INFORM OF FIRE LANES AS REQUIRED BY FIRE DEPARTMENT.
- CONSTRUCTION DOCUMENTS PERTAINING TO THE LANDSCAPE AND IRRIGATION OF THE ENTIRE PROJECT SITE SHALL BE SUBMITTED TO THE BUILDING DEPARTMENT AND APPROVED BY PUBLIC FACILITIES DEVELOPMENT PRIOR TO ISSUANCE OF BUILDING PERMITS.
- PRIOR TO FINAL CITY INSPECTION, THE LANDSCAPE ARCHITECT SHALL SUBMIT A CERTIFICATE OF COMPLETION TO PUBLIC FACILITIES DEVELOPMENT.
- ALL LANDSCAPE AND IRRIGATION DESIGNS SHALL MEET CURRENT CITY STANDARDS AS LISTED IN GUIDELINES OR AS OBTAINED FROM PUBLIC FACILITIES DEVELOPMENT.
- LANDSCAPED AREAS SHALL BE DELINEATED WITH A MINIMUM SIX INCHES (6") HIGH CURB.
- APP.
- WITH APP.

ZONING
 ZONING DESIGNATION - LIGHT INDUSTRIAL

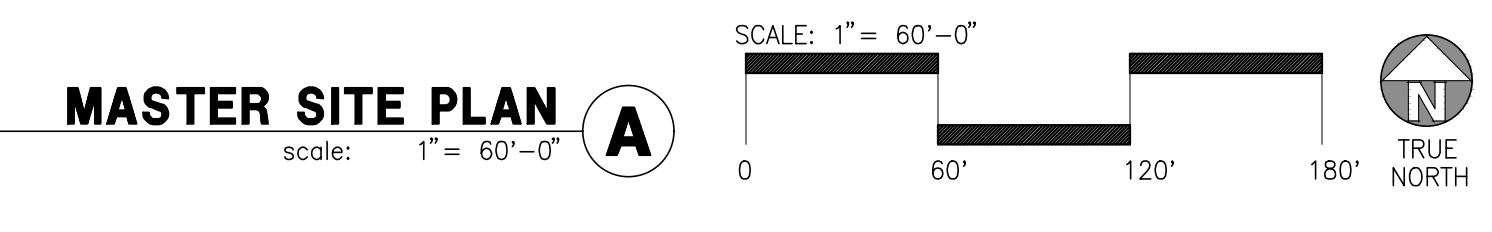
APPLICANT
 HPA, INC.
 18831 BARDEEN AVE SUITE 100
 IRVINE CA 92612
 TEL: (949) 862-2127
 CONTACT: YUNXUAN GUO

APPLICANT'S REPRESENTATIVE
 HPA, INC.
 18831 BARDEEN AVE SUITE 100
 IRVINE CA 92612
 TEL: (949) 862-2127
 CONTACT: YUNXUAN GUO

SITE LEGEND

	LANDSCAPED AREA		LIGHT STANDARD
	AC. PAVING - SEE "C" DRWS. FOR THICKNESS		EXISTING PUBLIC FIRE HYDRANT
	CONCRETE PAVING - SEE "C" DRWS. FOR THICKNESS		PRIVATE FIRE HYDRANT - APPROXIMATE LOCATION
	CHAIN LINK FENCE		PEDESTRIAN WALKWAY WITH LED WARNING LIGHTS
	PROPERTY LINE		CATCH BASIN, APPROX. LOCATION. SEE CIVIL DRWS.
	STANDARD PARKING STALL (9' x 19')		WATER LINE SEE CIVIL DRWS
	HANDICAP PARKING STALL (9' x 19')		GAS LINE SEE CIVIL DRWS
	PATH OF TRAVEL		SEWER LATERAL SEE CIVIL DRWS.

Existing Grade: 1413.5
Proposed Grade: 1421.6
Proposed Building Height: 36'
Proposed Roof Elevation: 1457.6
**Note: small sections of parapet walls to up to 43' tall*



HPA architecture

hpa, inc.
 18831 bardeen avenue - ste. #100
 irvine, ca 92612
 tel: 949-863-1770
 fax: 949-863-0851
 email: hpa@hparchs.com

LICENSED ARCHITECT
 YOUNG NAM
 3-31-21
 RENEWAL DATE

Owner:

RICHLAND COMMUNITIES

RICHLAND
 3161 Michelson Drive, Suite 425,
 Irvine, CA 92615
 tel: 714.932.0650

Project:

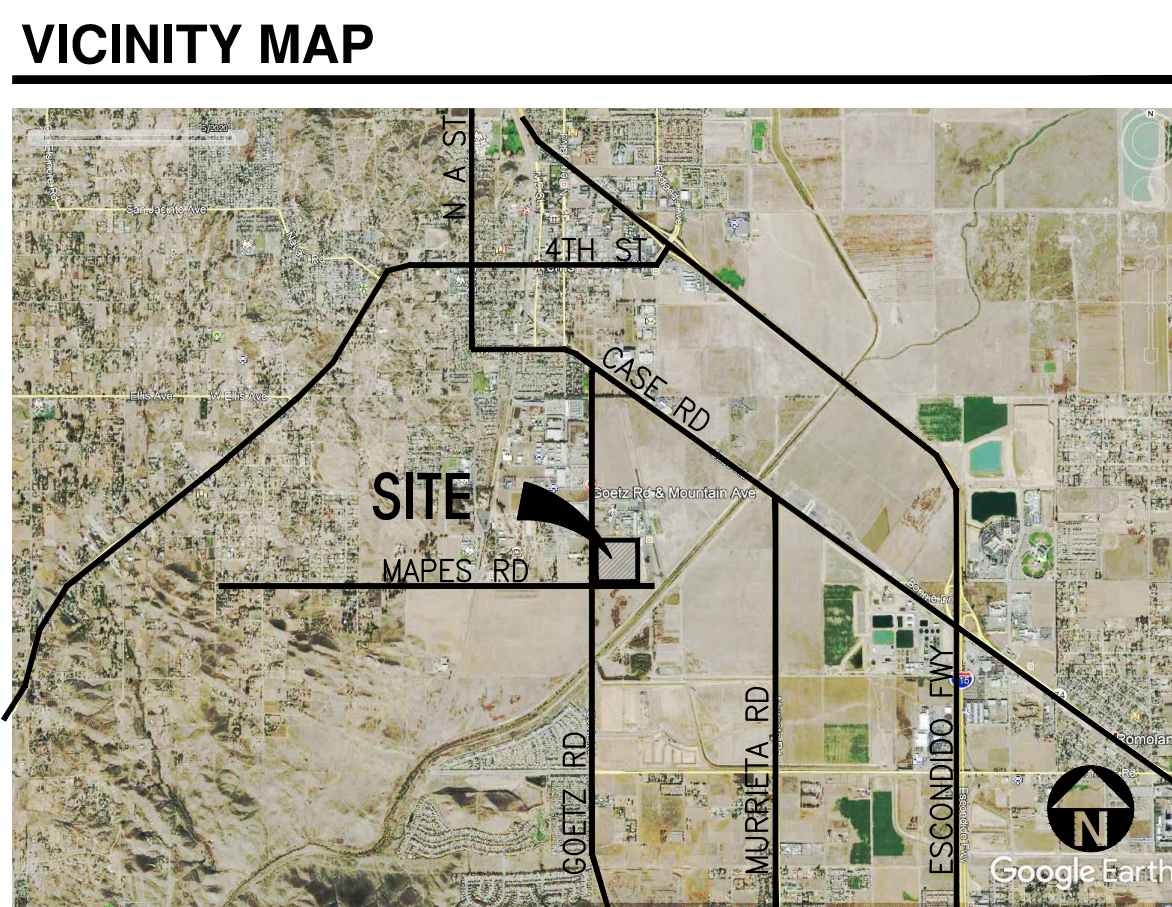
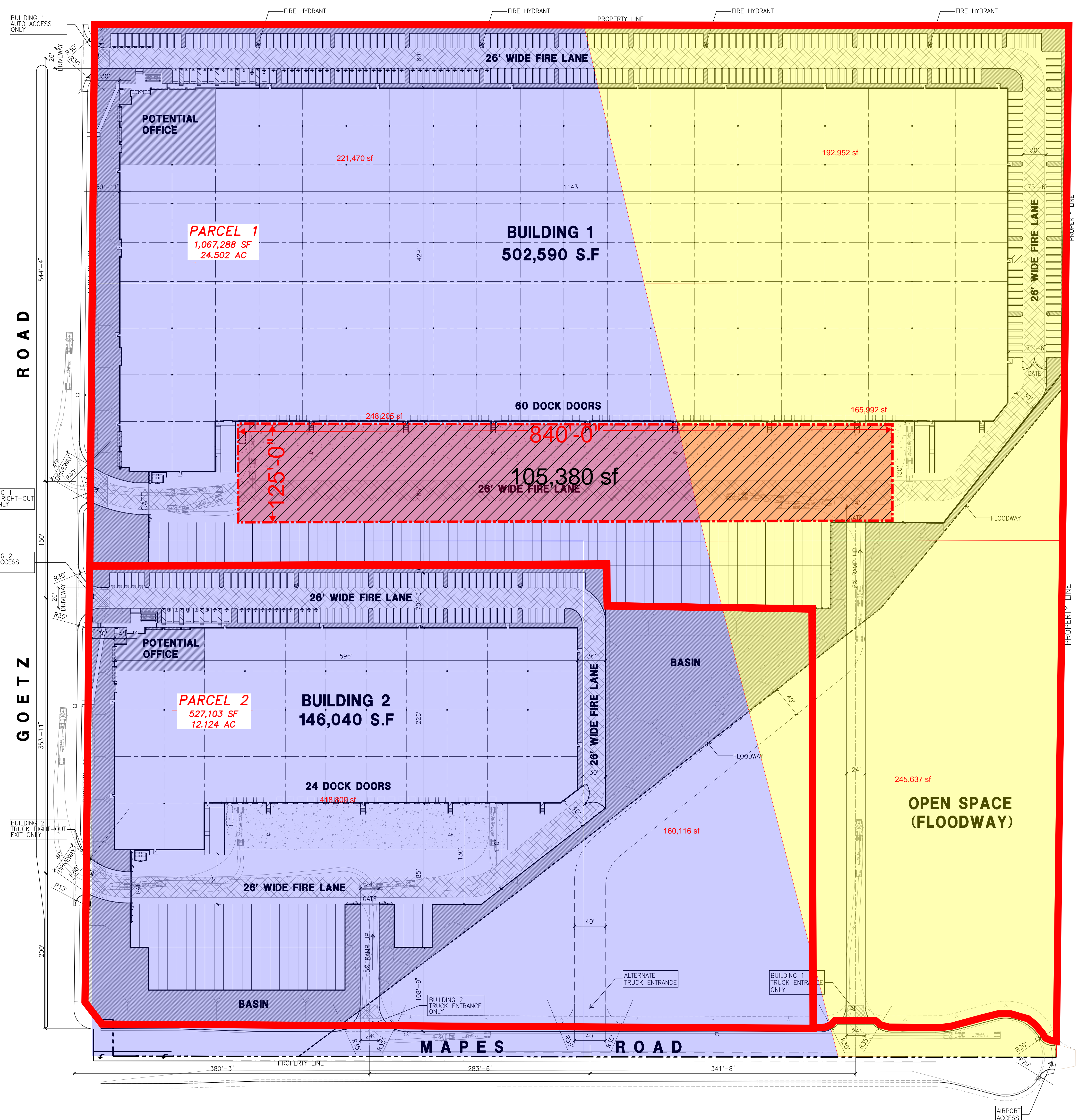
MAPES RD AND GOETZ RD.
 PERRIS, CA

Consultants:

CIVIL STRUCTURAL SB&O
 MECHANICAL
 PLUMBING
 ELECTRICAL EMERALD DESIGN
 LANDSCAPE
 FIRE PROTECTION
 SOILS ENGINEER

Title: MASTER SITE PLAN

Project Number: 20219
 Drawn by: Y.G.
 Date: 03/31/22
 Revision:



PROPERTY OWNER
 RICHLAND
 3161 MICHELSON DRIVE, SUITE 425, IRVINE, CA 92615
 CONTACT: DEREK BARBOUR
 TEL: 714.932.0650
 EMAIL: DBARBOUR@RICHLANDCOMMUNITIES.COM

ADDRESS OF THE PROPERTY
 MAPES RD. AND GOETZ RD.
 CITY OF PERRIS

ASSESSOR'S PARCEL NUMBER
 330-100-005 330-100-027
 330-100-006 330-100-025

HPA
 architecture

hpa, inc.
 18831 bardeen avenue - ste. #100
 irvine, ca
 92612
 tel: 949-863-1770
 fax: 949-863-0851
 email: hpa@hparchs.com

LICENSED ARCHITECT
 YOUNG NAM
 6-23-15
 9-31-21
 RENEWAL DATE
 STATE OF CALIFORNIA

Owner:

RICHLAND COMMUNITIES

RICHLAND
 3161 Michelson Drive, Suite 425,
 Irvine, CA 92615
 tel: 714.932.0650

Project:

MAPES RD AND
 GOETZ RD.

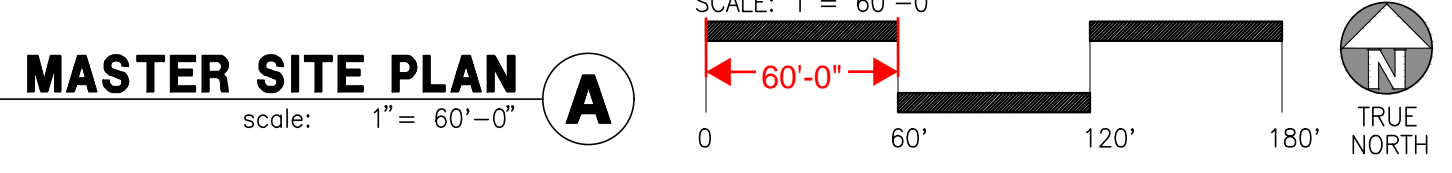
PERRIS, CA

Consultants:

CIVIL	SB&O
STRUCTURAL	-
MECHANICAL	-
PLUMBING	-
ELECTRICAL	-
LANDSCAPE	EMERALD DESIGN
FIRE PROTECTION	-
SOILS ENGINEER	-

Title: MASTER SITE PLAN

Project Number: 20219
Drawn by: Y.G.
Date: 05/10/22
Revision:



OPEN LAND AREA SUMMARY
 Developed Site Area within Zone D = 23.5 AC (1,023,660 SF)
 Open Land Area Required (10%) = 2.35 AC (102,366 SF)
 Open Land Area Provided = 105,380 SF

OPEN LAND AREA:
 SHALL HAVE A MINIMUM DIMENSION OF 75' X 300' AND BE FREE OF MOST STRUCTURE AND OTHER MAJOR OBSTACLES SUCH AS WALLS, LARGE TREES OR POLES (GREATER THAN 4" DIA. MEASURED 4 FEET ABOVE THE GROUND), AND OVERHEAD WIRES.

NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. **Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org.** The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893.**

The City of Perris Planning Department should be contacted on non-ALUC issues. For more information please contact City of Perris Planner Alfredo Garcia at (951) 943-5003.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to prull@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please contact Barbara Santos at (951) 955-5132.

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside California

DATE OF HEARING: June 9, 2022

TIME OF HEARING: 9:30 A.M.

CASE DESCRIPTION:

ZAP1026PV22 – Richland Developers, Inc (Representative: Derek Barbour) – City of Perris Case No. DPR 22-00002 (Development Plan Review), TPM38446 (Tentative Parcel Map). A proposal to construct two industrial buildings with mezzanines totaling 659,130 square feet on 37.55 acres, located on the northeast corner of Goetz Road and Mapes Road The applicant also proposes to divide the site into two parcels (Airport Compatibility Zones B2 and D of the Perris Valley Airport Influence Area, and Zone E of March Air Reserve Base/Inland Port Airport Influence Area).



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC STAFF ONLY

ALUC Case Number: _____

Date Submitted: _____

AIA: _____

Zone: _____

Public Hearing _____

Staff Review _____

Applicant

Applicant

Full Name: _____

Applicant Address: _____

Phone: _____ Email: _____

Representative/ Property Owner Contact Information

Representative: _____ Email: _____

Phone: _____

Address: _____

Property Owner: _____ Email: _____

Phone: _____

Address: _____

Local Jurisdiction Agency

Agency Name: _____ Phone: _____

Staff Contact: _____ Email: _____

Address: _____ : _____ :

Local Agency Case No.: _____

Project Location

Street Address: _____ Gross Parcel Size.: _____

Assessor's Parcel No.: _____

Solar

Is the project proposing solar Panels? Yes

No

If yes, please provide solar glare study.
(only if in Zone C or higher)

Data

Site Elevation:(above
mean sea level) _____

Height of Building or
structures: _____

What type of drainage basins are
being proposed and the square
footage: _____

Notice

A. NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. REVIEW TIME: Estimated time for “staff level review” is approximately 30 days from date of submittal. Estimated time for “commission level review” is approximately 45 days from date of a complete application submittal to the next available commission hearing meeting.

C. SUBMISSION PACKAGE:

Please submit all application items DIGITALLY via USB or CD:

- Completed ALUC Application Form
- Plans Package: site plans, floor plans, building elevations, grading plans, subdivision maps
- Exhibits of change of zone, general plan amendment, specific plan amendment
- Project description of existing and proposed use

Additionally, please provide:

- ALUC fee payment (Checks made out to Riverside County ALUC)
- Gummed address labels of all surrounding property owners within a 300-foot radius of project site. (Only required if the project is scheduled for a public hearing).

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM: 3.1

HEARING DATE: July 14, 2022

CASE NUMBER: ZAP1104RI22 – Kienle & Kienle Investments
(Representative: Adkan Engineers)

APPROVING JURISDICTION: City of Riverside

JURISDICTION CASE NO: PR2021-001208 (Change of Zone, Conditional Use Permit)

LAND USE PLAN: 2005 Riverside Municipal Airport Land Use Compatibility Plan

Airport Influence Area: Riverside Municipal Airport

Land Use Policy: Airport Compatibility Zones E

Noise Levels: Below 55 CNEL contour from aircraft noise

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends that the Change of Zone be found **CONSISTENT** with the 2005 Riverside Municipal Airport Land Use Compatibility Plan, and that the Conditional Use Permit also be found **CONSISTENT**, subject to the conditions included herein.

PROJECT DESCRIPTION: The applicant proposes to convert an existing 1,180 square foot single family residence into an office building on 1.44 acres. The applicant also proposes to change the site's zoning from R-1-7000 (Single Family Residential Zone) to O (Office Zone).

PROJECT LOCATION: The site is located at 8568 Indiana Ave, northerly of Patricia Way, westerly of Vance Street, and easterly of Bernard Street approximately 10,185 feet southwesterly of the westerly terminus of Runway 9-27 at Riverside Municipal Airport.

BACKGROUND:

Non-Residential Intensity: Pursuant to the Riverside Municipal Airport Land Use Compatibility Plan, the project site is located within Compatibility Zone E, where non-residential intensity is not restricted.

Prohibited and Discouraged Uses: The applicant does not propose any uses specifically prohibited or discouraged in Compatibility Zone E.

Noise: The site is located outside the Riverside Municipal Airport Compatibility Plan 55 CNEL

contour relative to aircraft noise contour. Therefore, no special measures to mitigate aircraft noise are required at this location.

Part 77: The elevation of Runway 9-27 at its easterly terminus is 815.8 feet above mean sea level (AMSL). At a distance of approximately 10,185 feet from the project to the nearest point on the runway, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 918 feet AMSL. The site's finished floor elevation is 822 feet AMSL and building height is 12 feet, resulting in a top point elevation of 834 feet AMSL. Therefore, review of the building for height/elevation reasons by the FAA Obstruction Evaluation Service (FAAOES) is not required.

Open Area: The site is located within Compatibility Zone E of the Riverside Municipal Airport Influence Area, which does not require projects to designate area as ALUC qualifying open area that could potentially serve as emergency landing areas.

Change of Zone: The applicant proposes to change the site's zoning from R-1-7000 (Single Family Residential) to O (Office Zone) to allow for office building use at this site. The amendments would be as, or more consistent with the Compatibility Plan as long as the underlying development is consistent with the compatibility criteria.

CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses are prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, outdoor production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Any use which results in a hazard to flight, including physical (e.g., tall objects),

visual, and electronic forms of interference with the safety of aircraft operations.

3. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property.
4. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the stormwater basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

**THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS**

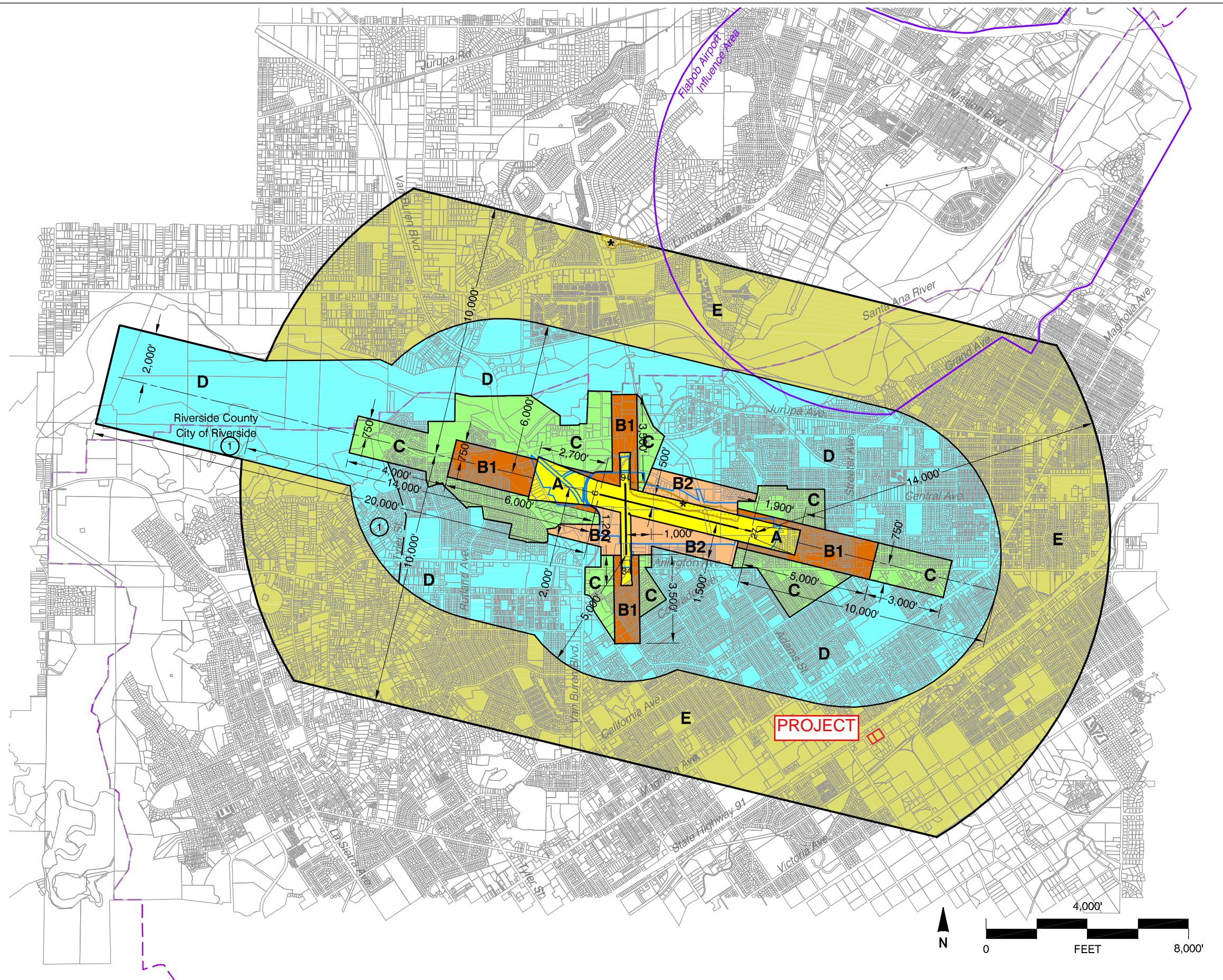
**PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES**



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



Legend

- Compatibility Zones**
- Airport Influence Area Boundary
 - Zone A
 - Zone B1
 - Zone B2
 - Zone C
 - Zone D
 - Zone E
 - Height Review Overlay Zone

- Boundary Lines**
- Airport Property Line
 - City Limits

Note
 Airport influence boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

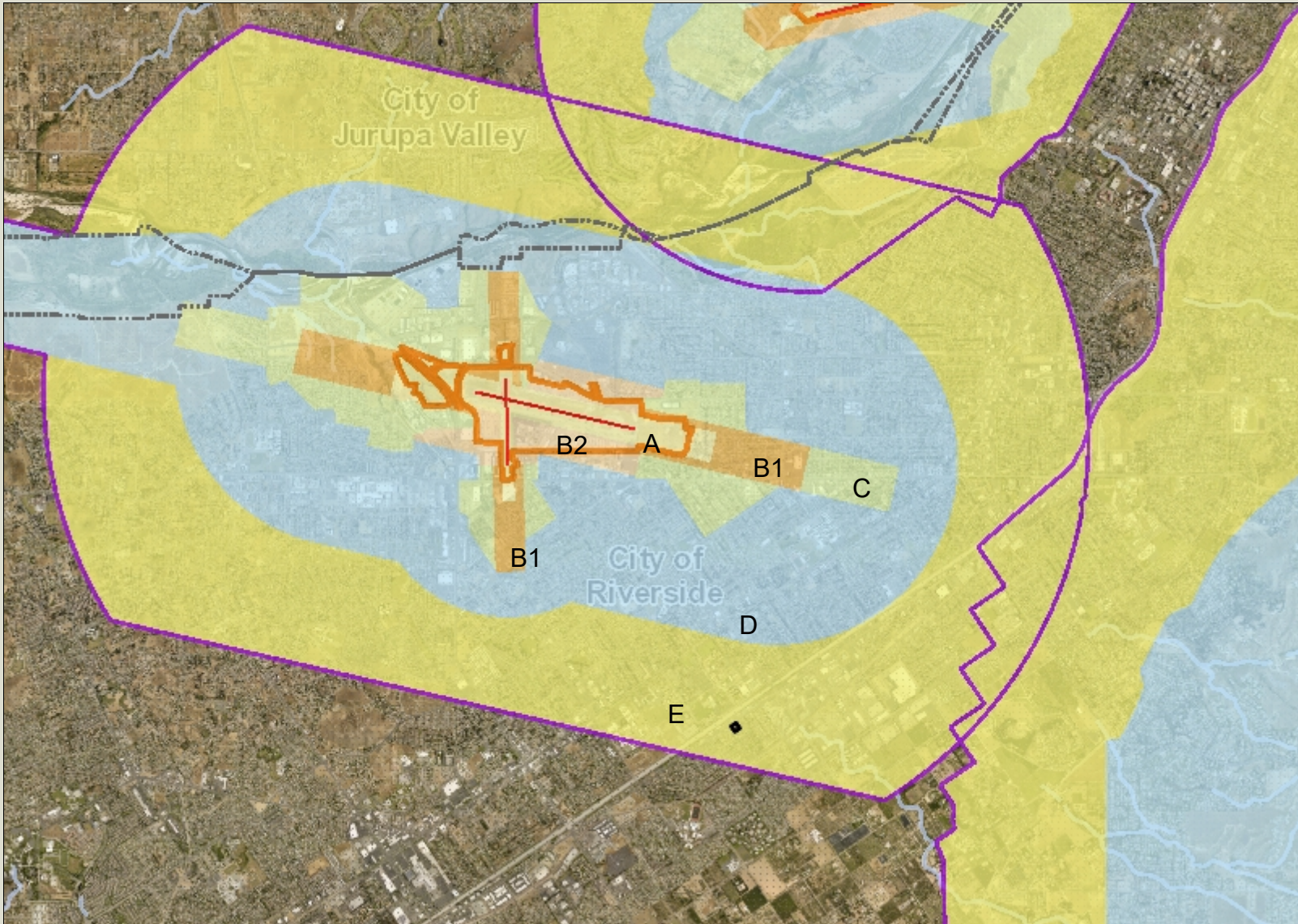
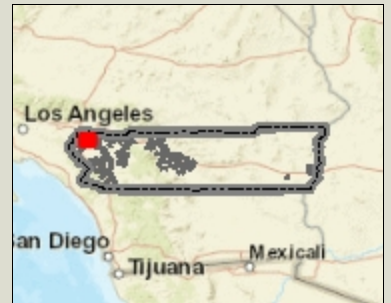
See Chapter 2, Table 2A for compatibility criteria associated with this map. See Section RI.2 for special exceptions to the Table 2A criteria.

Riverside County
Airport Land Use Commission
Riverside County
Airport Land Use Compatibility Plan
Policy Document
 (Adopted March 2005)

Map RI-1

Compatibility Map
Riverside Municipal Airport

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.



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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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0 385 770 Feet

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Notes

Map My County Map



Legend

-  Parcels
-  County Centerline Names
-  County Centerlines
-  Blueline Streams
-  City Areas
-  World Street Map



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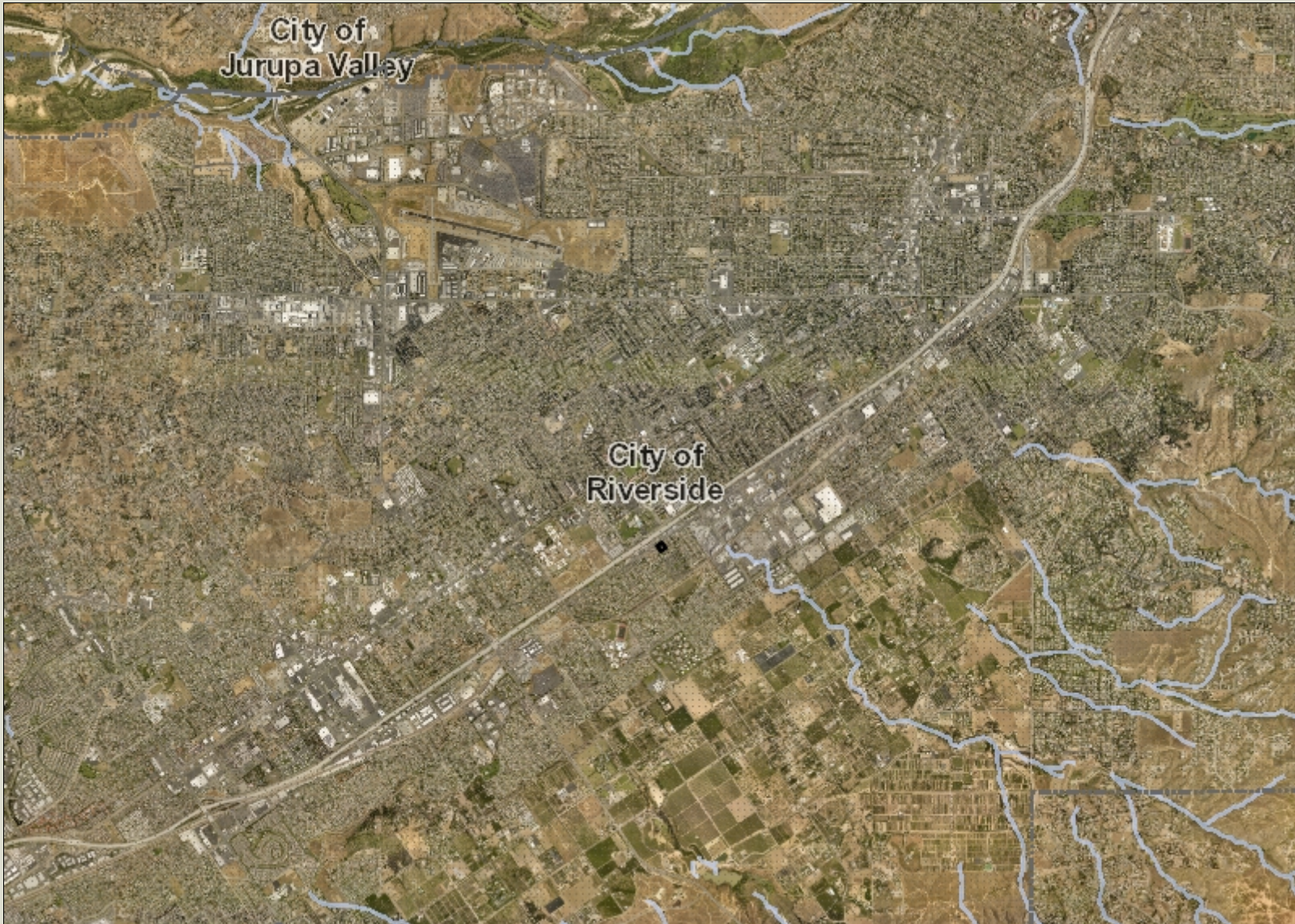
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Notes

Map My County Map



Legend

-  Blueline Streams
-  City Areas
-  World Street Map



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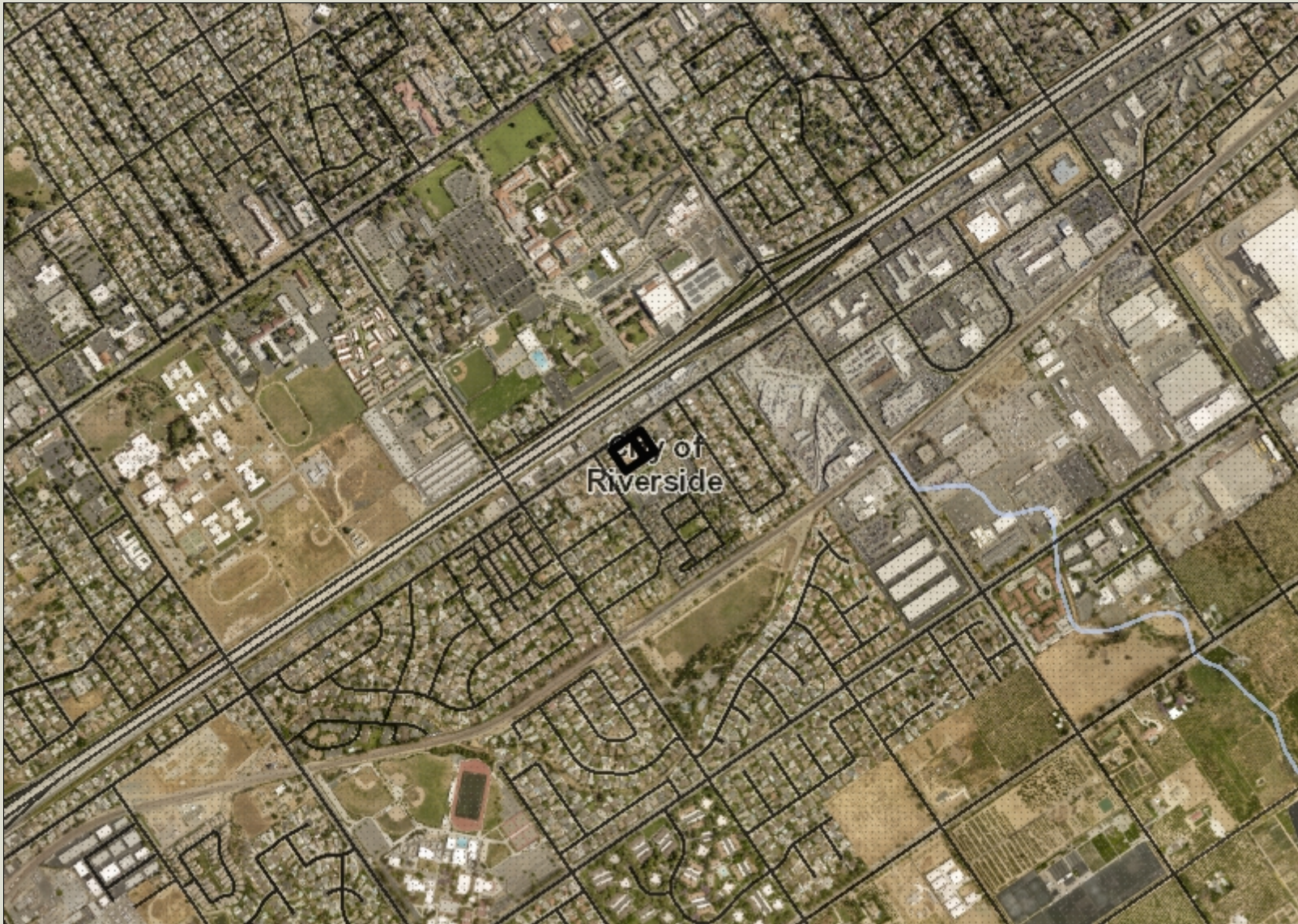


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Notes

Map My County Map



Legend

- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



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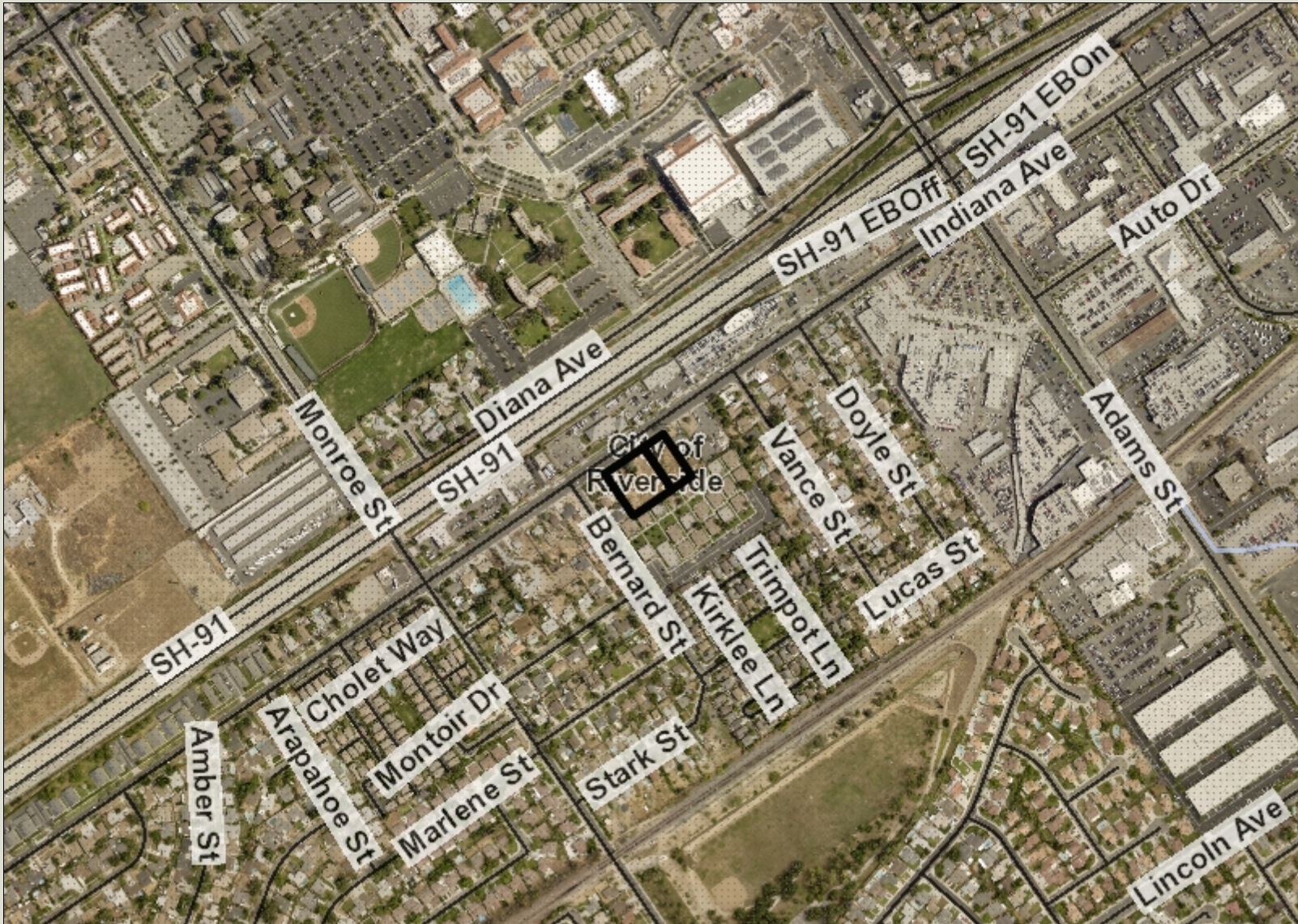
Notes

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539

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Map My County Map



Legend

- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

Notes

0 770 1,539 Feet

REPORT PRINTED ON... 5/25/2022 2:47:28 PM

© Riverside County GIS

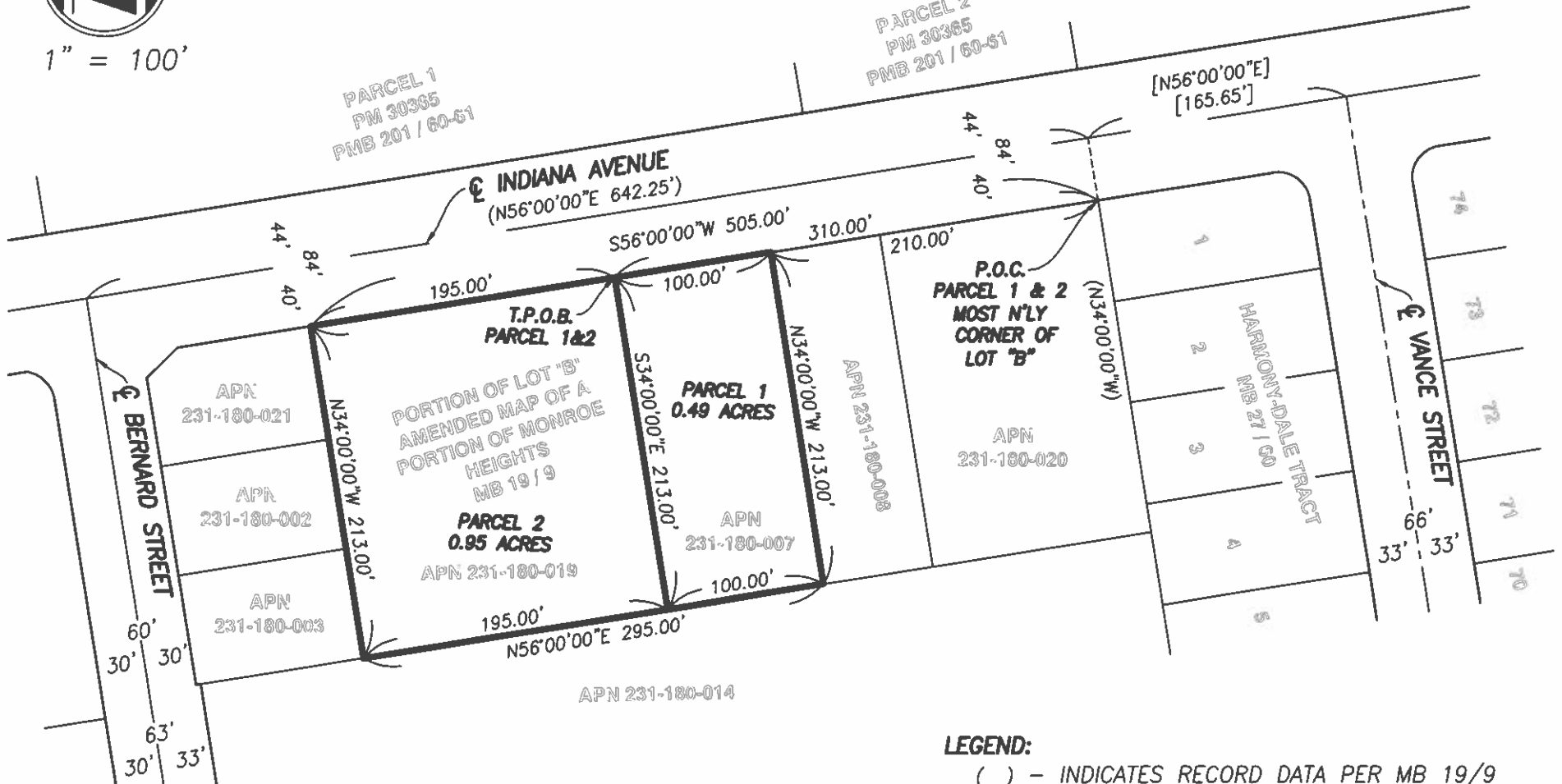


1" = 100'

FROM: R-1-7000 - SINGLE FAMILY RESIDENTIAL ZONE
TO: O - OFFICE ZONE

PARCEL 1
PM 30365
PMB 201 / 60-61


PARCEL 2
PM 30365
PMB 201 / 60-61



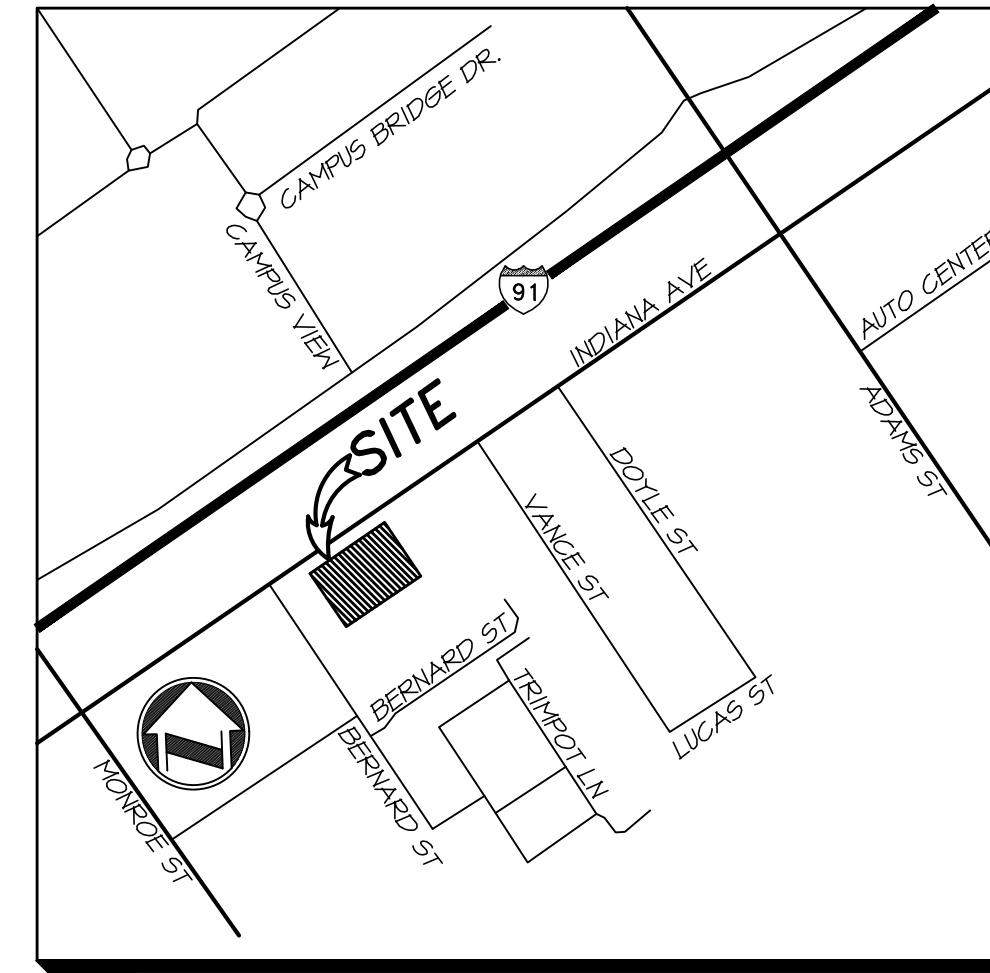
LEGEND:

- () - INDICATES RECORD DATA PER MB 19/9
- [] - INDICATES RECORD DATA PER MB 27/60



PREPARED BY: adkan ENGINEERS Civil Engineering • Surveying • Planning 6820 Airport Drive, Riverside, CA 92504 Tel:(951) 688-0241 • Fax:(951) 688-0599	05-10-2022	KIENLE & KIENLE - CHANGE OF ZONE
APPROVED BY:  MITCHELL JAY ADKISON, PLS 8936	PORTION OF LOT "B" MB 19/9	
	JN. 9665	SHEET 2 OF 2

IN THE CITY OF RIVERSIDE, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA
8568 INDIANA AVENUE
WALTERS BUSINESS CENTER
SITE PLAN WITH GRADING



VICINITY MAP
 (N.T.S.)
T3SR5W SEC 8

OWNER/APPLICANT	ENGINEER
KIBBLE & KIBBLE INVESTMENTS 3213 ADAMS STREET RIVERSIDE, CA 92504 (951) 688-3332	adkan ENGINEERS 6879 AIRPORT DRIVE RIVERSIDE, CA 92504 951-688-0241
ZONING & LAND USE	R-1-1000-SINGLE FAMILY RESIDENTIAL
EXISTING ZONING:	O-OFFICE
EXISTING LAND USE:	CG-COMMERCIAL GENERAL
PROPOSED ZONING:	O-OFFICE
PROPOSED LAND USE:	O-OFFICE
UTILITY PURVEYORS	
WATER:	CITY OF RIVERSIDE
SEWER:	CITY OF RIVERSIDE
ELECTRIC:	RIVERSIDE PUBLIC UTILITIES
GAS:	SO. CAL. GAS COMPANY
SCHOOL DISTRICT:	RIVERSIDE UNIFIED SCHOOL DISTRICT

PROPERTY INFORMATION	ASSESSOR'S PARCEL NUMBER
TOTAL GROSS AREA: 1.44 ACRES	231-180-019, 231-180-007
TOTAL NET AREA: 1.42 ACRES	
OVERALL DIMENSIONS: 244' X 213'	

- PROJECT NOTES**
- THOMAS BROS. MAP PAGE T15 GRID B5.
 - ALL CUT SLOPES WILL BE 2:1 RATIO AND FILL SLOPES 2:1, UNLESS OTHERWISE NOTED.
 - SETBACKS OF SLOPES TO PROPERTY LINES SHALL CONFORM TO CBC 2019.
 - LOT DIMENSIONS SHOWN HEREON INCLUDE DIMENSIONS TO STREET R/W.
 - PROPERTY NOT LOCATED IN A FLOODPLAIN.
 - PROPERTY HAS LOW LIQUEFACTION POTENTIAL.
 - PROPERTY IS NOT IN A SUBSIDENCE AREA.
 - PROPERTY CONTAINS NO KNOWN WELLS.
 - PROPERTY IS NOT IN A FAULT ZONE.
 - PROPERTY IS NOT IN A FAULT LINE.
 - SETBACKS TO ALL SITE BOUNDARIES WILL COMPLY WITH THE 2019 CBC.
 - THERE ARE TWO EXISTING STRUCTURES LOCATED ON SITE. THE 1180 SF BUILDING IS TO REMAIN FOR FUTURE OFFICE USE AND THE 547 SF GARAGE IS TO BE REMOVED.
 - THERE ARE NO PROPOSED WALLS/FENCES WITHIN THE FRONT YARD SETBACK (15' FROM THE ULTIMATE RIGHT OF WAY) WALLS/FENCES LOCATED OUTSIDE THE SETBACK ARE LIMITED TO 6 FEET IN HEIGHT.

LEGEND

SD = STORM DRAIN	EX = EXISTING
TC = TOP OF CURB	MH = MAN HOLE
FS = FINISH SURFACE	FE = PAD ELEVATION
SHW = SEWER	PROP = PROPOSED
TF = TOP OF FOOTING	S/W = SIDEWALK
CL = CENTER LINE	() = EXISTING ELEVATION
FG = FINISHED GROUND	C+G = CURB AND GUTTER
WTR = WATER	S.F. = SQUARE FEET
EP = EDGE OF PAVEMENT	CF = CURB FACE
PL = PROPERTY LINE	TE = TRASH ENCLOSURE
R/W = RIGHT OF WAY	(X) = PARKING ROW COUNT
O.S. = OPEN SPACE	

PARKING REQUIREMENTS

PARKING STANDARD FOR BUSINESS OFFICE: 1 SPACE / 200 SF
 BUILDING AREA: 1180 SF
 REQUIRED PARKING: 6 SPACES
 PROVIDED PARKING: 134 SPACES

BENCHMARK

POINT ID: F5-J3
 DATUM: NAD83
 DESCRIPTION: P.C. NAIL AND CITY ENGINEER TAG IN STREET LIGHT BASE ALONG NORTHERLY CURB OF INDIANA AVENUE, 15 FEET WEST OF VANCE STREET. (SET AS OPART OF THE AUTO CENTER RENOVATION PROJECT)

ELEVATION: 824.075

TRACT BOUNDARY

PROPOSED CURB & GUTTER

PAD

LOT LINE

MAJOR CONTOUR

MINOR CONTOUR

ALUG LIMIT

CENTER LINE

PROP. EASEMENT

EXISTING WATER

EXISTING SEWER

EXISTING GAS

EXISTING WALL

EXISTING CURB & GUTTER

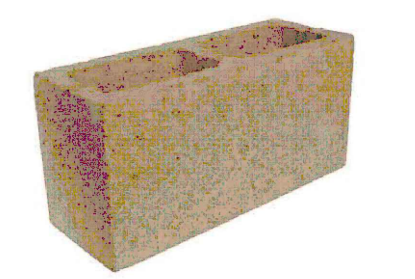
EXISTING STORM DRAIN

CONCRETE

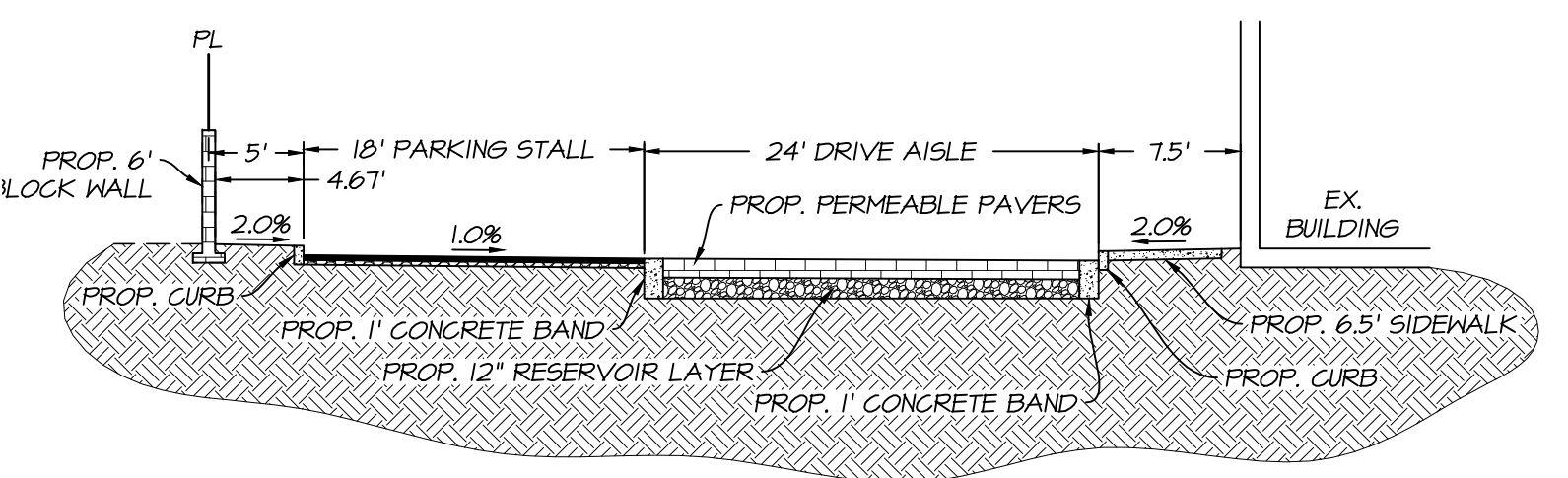
LANDSCAPING

ASPHALT

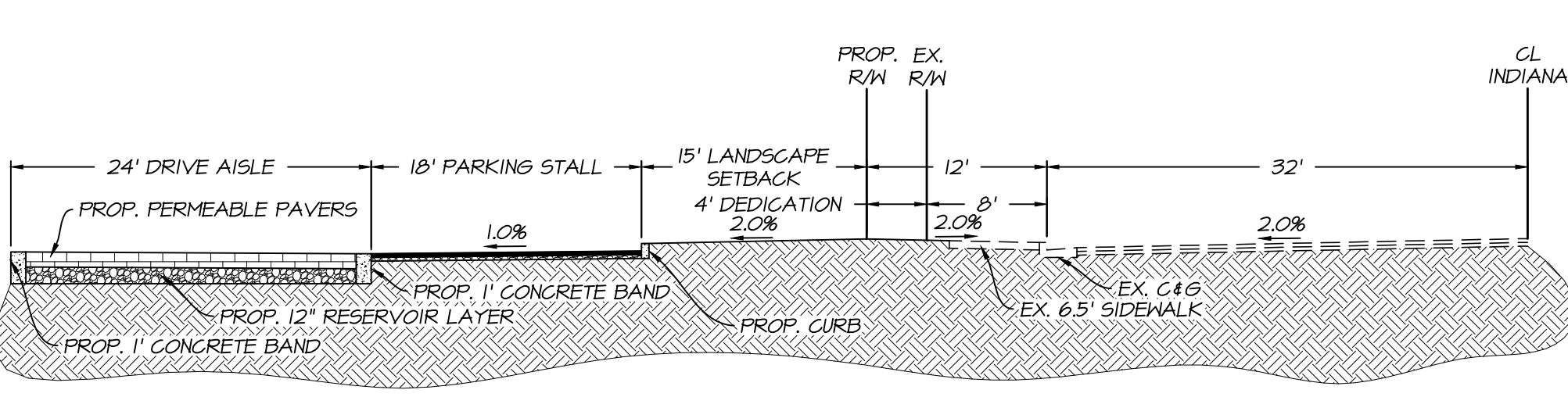
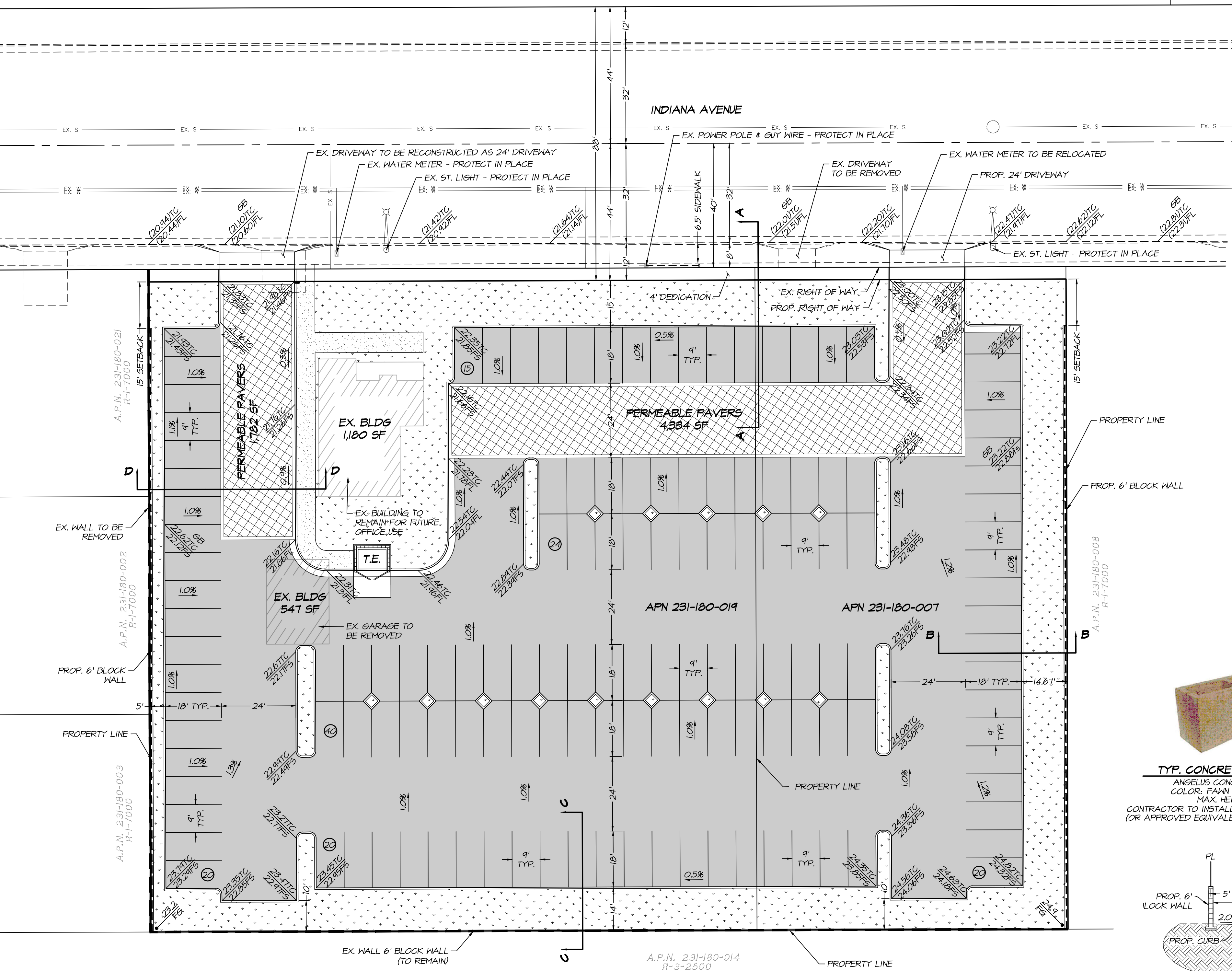
EXISTING BUILDING



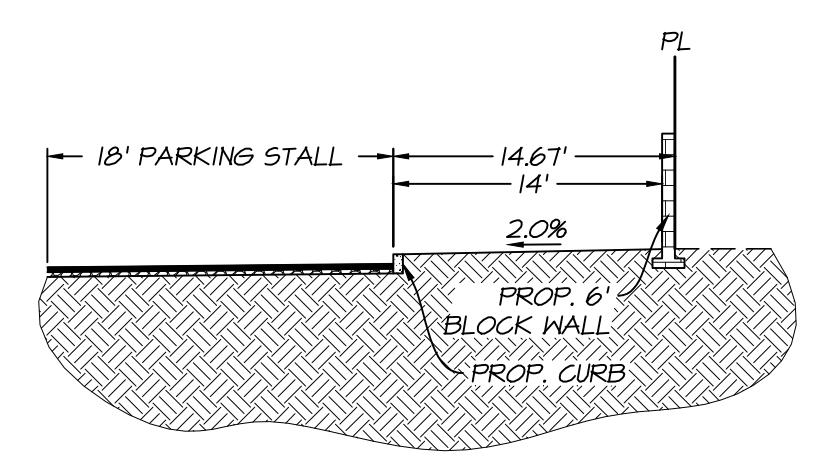
TYP. CONCRETE WALL BLOCK
 ANGELUS CONCRETE BLOCK
 COLOR: FAWN SIZE: 6"X8"X16"
 MAX. HEIGHT: 6.0'
 CONTRACTOR TO INSTALL MATCHING 2" CAP AT TOP (OR APPROVED EQUIVALENT BY OTHER MANUFACTURER)



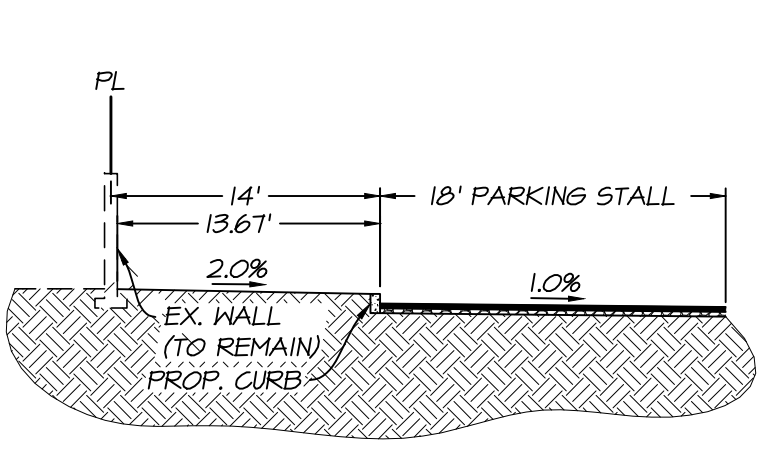
SECTION D-D
 SCALE: 1"=10'



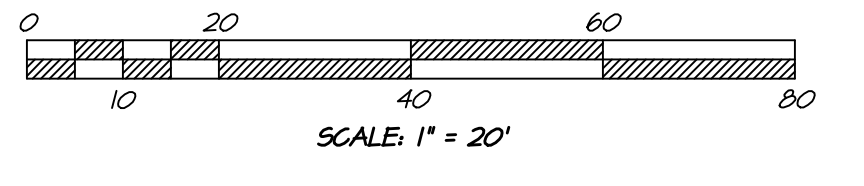
SECTION A-A
 SCALE: 1"=10'



SECTION B-B
 SCALE: 1"=10'



SECTION C-C
 SCALE: 1"=10'



SCALE: 1" = 20'

REVISIONS	
NO.	DESCRIPTION

8568 INDIANA AVENUE
 WALTERS BUSINESS CENTER
 CONCEPTUAL GRADING PLAN
 PREPARATION DATE: FEBRUARY 2022

adkan ENGINEERS
 Civil Engineering - Surveying - Planning
 6879 Airport Drive, Riverside, CA 92504
 Tel: (951) 688-0241 • Fax: (951) 688-0599

FILE LOCATION: U:\PROJECTS\KIBBLE INVESTMENTS\18665\PLANNING\CONCEPTUAL GRADING\18665_CG01.DWG

NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. **Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org.** The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893.**

The City of Riverside Planning Department should be contacted on non-ALUC issues. For more information, please contact City of Riverside Planner Candice Assadzadeh at (951) 688-0241.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to prull@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please contact Barbara Santos at (951) 955-5132.

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside California

DATE OF HEARING: July 14, 2022

TIME OF HEARING: 9:30 A.M.

CASE DESCRIPTION:

ZAP1104RI22 – Kienle & Kienle Investments (Representative: Adkan Engineers) City of Riverside Case No. PR2021-001208 (Change of Zone, Conditional Use Permit). A proposal to convert an existing 1,180 square foot single family residence into an office building on 1.44 acres, located at 8568 Indiana Ave, northerly of Patricia Way, westerly of Vance Street, and easterly of Bernard Street. The applicant also proposes to change the site's zoning from R-1-7000 (Single Family Residential Zone) to O (Office Zone) (Airport Compatibility Zone E of the Riverside Municipal Airport Influence Area).



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC STAFF ONLY

ALUC Case Number: ZAP1104RI22

Date Submitted: 5-19-22

AIA:

Riverside Municipal

Zone: E

Public Hearing

Staff Review

Applicant

Applicant

Full Name: Kienle & Kienle Investments Attn: Steve Kienle

Applicant Address: 3213 Adams Street Riverside Ca 92504

Phone: 951-688-3332

Email: skienle@waltsag.com

Representative/ Property Owner Contact Information

Representative: Adkan Engineers

Email: Madkison@adkan.com

Mitch Adkison

Phone: 951-688-0241

Address: 6879 Airport Drive Riverside Ca 92504

Property

Owner:

Kienle & kienle Investments attn: Steve Kienle

Email: skienle@waltsag.com

Phone: 951-68-3332

Address: 8568 Indiana Avenue Riverside Ca

Local Jurisdiction Agency

Agency

Name:

City of Riverside

Phone: 951-826-5371

Staff Contact: Candice Assadzadeh

Email: CAssadzadeh@riversideca.gov

Address: 3900 Main street Riverside CA 3rd Floor

Local Agency

Case No.:

PR-2021-001208

Project Location

Street

Address:

8568 Indiana Ave Riverside CA

Gross Parcel Size: 1.44 acres

Assessor's Parcel No.: 231-180-019, 231-180-007

Solar

Is the project proposing solar Panels? Yes

No

If yes, please provide solar glare study.
(only if in Zone C or higher)

Data

Site Elevation:(above mean sea level) 822.0

Height of Building or structures: 12 ft

What type of drainage basins are being proposed and the square footage: None Proposed

Notice

A. NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. REVIEW TIME: Estimated time for “staff level review” is approximately 30 days from date of submittal. Estimated time for “commission level review” is approximately 45 days from date of a complete application submittal to the next available commission hearing meeting.

C. SUBMISSION PACKAGE:

Please submit all application items DIGITALLY via USB or CD:

- Completed ALUC Application Form
- Plans Package: site plans, floor plans, building elevations, grading plans, subdivision maps
- Exhibits of change of zone, general plan amendment, specific plan amendment
- Project description of existing and proposed use

Additionally, please provide:

- ALUC fee payment (Checks made out to Riverside County ALUC)
- Gummed address labels of all surrounding property owners within a 300-foot radius of project site. (Only required if the project is scheduled for a public hearing).

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM: 3.2

HEARING DATE: July 14, 2022

CASE NUMBER: ZAP1101PS22 – Casa Verona, LLC (Representative: Temple Construction, Inc.)

APPROVING JURISDICTION: City of Palm Springs

JURISDICTION CASE NO: 5.1527 (Change of Zone), TTM38042 (Tentative Tract Map)

LAND USE PLAN: 2005 Palm Springs International Airport Land Use Compatibility Plan

Airport Influence Area: Palm Springs International Airport

Land Use Policy: Compatibility Zone D

Noise Levels: Below 60 CNEL

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends that the Commission find the Change of Zone CONSISTENT with the 2005 Palm Springs Airport Land Use Compatibility Plan, and find the Tentative Tract Map CONSISTENT subject to the conditions included herein.

PROJECT DESCRIPTION: A proposal to divide 6.44 gross acres into 31 single-family residential lots and one retention basin lot. The applicant also proposes changing the site's zoning from R-1-D (Single Family Residential minimum 7,500 square foot lot size zone) to R-1-E (Small-lot single-family residential zone).

PROJECT LOCATION: The site is located northerly of Verona Road, easterly of Whitewater Club Drive, and westerly of the Whitewater Channel, approximately 4,662 feet northeasterly of the northerly end of Runway 13L-31R at Palm Springs International Airport.

BACKGROUND:

Residential Density: Pursuant to the Palm Springs International Airport Land Use Compatibility Plan, the project site is located within Compatibility Zone D, which restricts residential density to either below 0.2 dwelling units per acre, or above 3.0 dwelling units per acre (per Additional Policy #2.3).

The project proposes creating 31 residential lots on 6.44 gross acres, resulting in a density of 4.81 dwelling units per acre, which is consistent with Zone D residential density criteria of either below

0.2 dwelling units per acre or above 3.0 dwelling units per acre (Additional Policy #2.3).

Prohibited and Discouraged Uses: The applicant does not propose any new use specifically prohibited or discouraged in Compatibility Zone D of the Palm Springs International Airport Influence Area.

Noise: The Palm Springs International Airport Land Use Compatibility Plan depicts the site as being located below the 60 CNEL contour range from aircraft noise. Therefore, special measures to mitigate aircraft-generated noise would not be required.

Part 77: The elevation of Runway 13L-31R at its northerly terminus is 474.4 feet above mean sea level (AMSL). At a distance of approximately 4,662 feet from the runway to the project, Federal Aviation Administration (FAA) review would be required for any structures with a top point elevation exceeding 521.4 feet AMSL. The project's site elevation is 480 feet AMSL and the proposed maximum structure height is 30 feet, for a top point elevation of 510 feet AMSL. Therefore, review by the FAA Obstruction Evaluation Service for height/elevation reasons is not required.

Open Area: Compatibility Zone D requires 10% of the land area within major projects (10 acres or larger) be set aside as open area that could potentially serve as emergency landing areas. The proposed project is on 6.44 acres in area, therefore open area is not required.

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33C). The nearest portion of the project is located 4,662 feet from the runway, and therefore would be subject to the above requirement.

The project utilizes a bioretention basin which is permissible in Zone D within 10,000 feet of the airport only with appropriate criteria: basin remain less than 30 feet in length or width, and vegetation is selected carefully so as not to provide food, shelter nesting roosting, or water for wildlife. The project has been conditioned to be consistent with the basin criteria (as well as providing 48-hour draw down of the basin).

Change of Zone: The applicant also proposes changing the site's zoning from R-1-D (Single Family Residential minimum 7,500 square foot lot size zone) to R-1-E (Small-lot single-family residential zone). The proposed change would be as, or more consistent, with the Compatibility Plan than the existing designation, as long as the project is consistent with the underlying compatibility criteria.

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:

- (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. Such uses include landscaping utilizing water features, aquaculture, outdoor production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Highly noise-sensitive outdoor nonresidential uses.
 - (f) Any use which results in a hazard to flight, including physical (e.g. tall objects), visual, and electronic forms of interference with the safety of aircraft operations.
3. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property and be recorded as a deed notice.
 4. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the stormwater basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor

the stormwater basin.

6. Buildings shall be limited to a maximum top point elevation of 521 feet above mean sea level unless a "Determination of No Hazard to Air Navigation" letter authorizing a higher top point elevation has been issued by the Federal Aviation Administration Obstruction Evaluation Service.
7. During initial sales of properties within newly created subdivision, large airport related informational signs shall be installed and maintained by the developer. These signs shall be installed in conspicuous locations and shall clearly depict the proximity of the property to the airport and aircraft traffic pattern. The ALUC overflight informational brochure shall be provided to prospective purchasers showing the locations of aircraft flight patterns, the frequency of overflights, the typical altitudes of the aircraft, and the range of noise levels that can be expected from individual aircraft overflights, as well as Compatibility Factors exhibit from the Palm Springs International Airport Land Use Compatibility Plan.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

**THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS**

**PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES**



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____

<p>GENERAL INFORMATION</p> <ul style="list-style-type: none"> › <i>Airport Ownership:</i> City of Palm Springs › <i>Year Opened:</i> 1939 › <i>Property Size</i> <ul style="list-style-type: none"> › Fee title: 932 acres › Avigation easements: 16 acres › <i>Airport Classification:</i> Primary Commercial Service › <i>Airport Elevation:</i> 474 feet MSL 	<p>AIRPORT PLANNING DOCUMENTS</p> <ul style="list-style-type: none"> › <i>Airport Master Plan</i> <ul style="list-style-type: none"> › Adopted by City Council, May 2003 › <i>Airport Layout Plan Drawing</i> <ul style="list-style-type: none"> › Last updated, May 2003 › <i>FAR Part 150 Airport Noise Compatibility Program</i> <ul style="list-style-type: none"> › Approved by FAA, June 1994
<p>RUNWAY/TAXIWAY DESIGN</p> <p>Runway 13R-31L</p> <ul style="list-style-type: none"> › <i>Critical Aircraft:</i> DC-10, B-747 › <i>Airport Reference Code:</i> D-IV › <i>Dimensions:</i> 10,000 ft. long, 150 ft. wide <ul style="list-style-type: none"> › Runway 13R end displaced 3,000 ft. › Runway 31L end displaced 1,500 ft. › <i>Pavement Strength: (main landing gear configuration)</i> <ul style="list-style-type: none"> › 105,000 lbs (single wheel) › 200,000 lbs (dual wheel) › 330,000 lbs (dual-tandem wheel) › 800,000 lbs (double-dual-tandem-wheel) › <i>Average Gradient:</i> 0.8% (rising to north) › <i>Runway Lighting:</i> High-intensity edge lights (HIRL) › <i>Primary Taxiways:</i> Full-length parallel on both sides <p>Runway 13L-31R</p> <ul style="list-style-type: none"> › <i>Critical Aircraft:</i> Medium twin › <i>Airport Reference Code:</i> B-II › <i>Dimensions:</i> 4,952 ft. long, 75 ft. wide › <i>Pavement Strength: (main landing gear configuration)</i> <ul style="list-style-type: none"> › 12,500 lbs (single wheel) › 60,000 lbs (dual wheel) › <i>Average Gradient:</i> 0.9% (rising to north) › <i>Runway Lighting:</i> Medium-intensity edge lights (MIRL) › <i>Primary Taxiways:</i> Full-length parallel on east side 	<p>TRAFFIC PATTERNS AND APPROACH PROCEDURES</p> <ul style="list-style-type: none"> › <i>Airplane Traffic Patterns</i> <ul style="list-style-type: none"> › Runways 13L, 13R: Left traffic › Runways 31L, 31R: Right traffic › <i>Pattern Altitude:</i> 1,000 ft. AGL small aircraft, 1,500 ft. AGL others › <i>Instrument Approach Procedures (lowest minimums)</i> <ul style="list-style-type: none"> › Runway 31L VOR or GPS-B <ul style="list-style-type: none"> › Circling (1¼ mile visibility, 1,900 ft. descent height) › <i>Standard Inst. Departure Procedures (initial direction)</i> <ul style="list-style-type: none"> › Runways 13L/R: Climbing left turn to 040° › Runways 31L/R: Climbing right turn › <i>Visual Approach Aids</i> <ul style="list-style-type: none"> › Runway 13R: VASI (3.0°); REIL › Runway 31L: PAPI (3.0°); REIL › Runway 13L: PAPI (3.5°); REIL › Runway 31R: PAPI (3.5°); REIL › <i>Operational Restrictions / Noise Abatement Procedures</i> <ul style="list-style-type: none"> › Calm winds: Use Runway 13 › Noise-sensitive area all quadrants; use quiet flight procedures › Runways 13R, 31L thresholds displaced for noise abatement
<p>BUILDING AREA</p> <ul style="list-style-type: none"> › <i>Location:</i> South side and northwest along property line › <i>Aircraft Parking Capacity</i> <ul style="list-style-type: none"> › Hangar spaces: 75 (includes FBO, Skywest hangars) › Tiedowns: 90 › <i>Other Major Facilities</i> <ul style="list-style-type: none"> › Air traffic control tower › Pilots lounge › <i>Services</i> <ul style="list-style-type: none"> › Fuel: 100LL, Jet A (via truck, 6:00 a.m. to 10:00 p.m.) › Commercial airline service › Other: Aircraft rental & instruction; aircraft maintenance & modification; sightseeing tours 	<p>PLANNED FACILITY IMPROVEMENTS</p> <ul style="list-style-type: none"> › <i>Airfield</i> <ul style="list-style-type: none"> › Add approach light system to Runway 31L › Establish Rwy 31L Cat. I precision inst. approach › <i>Building Area</i> <ul style="list-style-type: none"> › Replace air traffic control tower › Expand terminal apron › <i>Property</i> <ul style="list-style-type: none"> › No planned acquisition

Exhibit PS-1

Airport Features Summary
Palm Springs International Airport

<p>BASED AIRCRAFT</p> <table border="1"> <thead> <tr> <th>Aircraft Type</th> <th>Current^a 2002 data</th> <th>Future^b 2025</th> </tr> </thead> <tbody> <tr><td>Single-Engine</td><td>99</td><td>152</td></tr> <tr><td>Twin-Engine Piston</td><td>20</td><td>35</td></tr> <tr><td>Turboprop</td><td>4</td><td>18</td></tr> <tr><td>Turbojet</td><td>2</td><td>11</td></tr> <tr><td>Helicopters</td><td>2</td><td>1</td></tr> <tr><td>Total</td><td>127</td><td>220</td></tr> </tbody> </table>	Aircraft Type	Current ^a 2002 data	Future ^b 2025	Single-Engine	99	152	Twin-Engine Piston	20	35	Turboprop	4	18	Turbojet	2	11	Helicopters	2	1	Total	127	220	<p>TIME OF DAY DISTRIBUTION</p> <table border="1"> <thead> <tr> <th>Airline</th> <th>Current^c</th> <th>Future^b</th> </tr> </thead> <tbody> <tr><td>Day</td><td>77%</td><td>76%</td></tr> <tr><td>Evening</td><td>14%</td><td>19%</td></tr> <tr><td>Night</td><td>9%</td><td>5%</td></tr> <tr><td>Other Airplanes</td><td></td><td></td></tr> <tr><td>Day</td><td>78%</td><td>no</td></tr> <tr><td>Evening</td><td>15%</td><td>change</td></tr> <tr><td>Night</td><td>7%</td><td></td></tr> <tr><td>Helicopters</td><td></td><td></td></tr> <tr><td>Day</td><td>81%</td><td>no</td></tr> <tr><td>Evening</td><td>15%</td><td>change</td></tr> <tr><td>Night</td><td>4%</td><td></td></tr> </tbody> </table>	Airline	Current ^c	Future ^b	Day	77%	76%	Evening	14%	19%	Night	9%	5%	Other Airplanes			Day	78%	no	Evening	15%	change	Night	7%		Helicopters			Day	81%	no	Evening	15%	change	Night	4%																												
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<p>Notes</p> <ul style="list-style-type: none"> ^a Source: Airport management records ^b Source: 2003 Airport Master Plan forecast for 2020 assumed as 2025 for compatibility planning purposes ^c Source: 2003 Airport Master Plan estimates 																																																																																					

Exhibit PS-3

Airport Activity Data Summary
Palm Springs International Airport

Presence of Aircraft Overflight: Palm Springs International Airport

EXPANDED BUYER AWARENESS MEASURES

As stipulated in the Riverside County Airport Land Use Compatibility Plan (ALUCP) for Palm Springs International Airport, any new single-family or multi-family residential development within the Palm Springs International Airport Influence Area (except those portions in Compatibility Zone E) shall be provided measures intended to ensure that prospective buyers or renters are informed about the presence of aircraft overflights of the property.

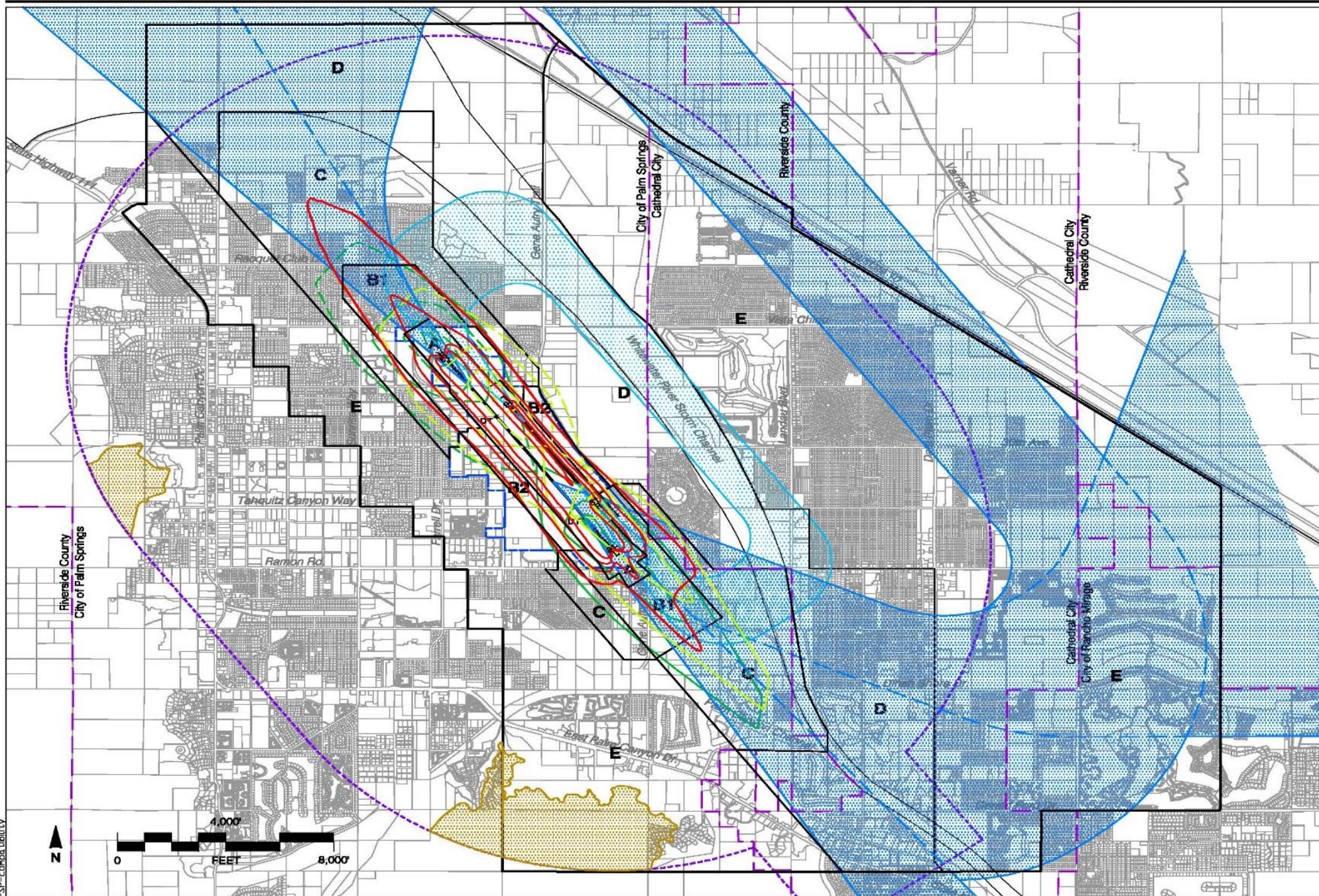
This brochure provides buyers or renters with information showing the locations of aircraft flight patterns, frequency of overflights, typical altitudes of the aircraft, and range of noise levels that can be expected from individual aircraft overflight.



For more information contact us:
Airport Land Use Commission
(951) 955-5132
www.rcaluc.org



FIND YOUR NEIGHBORHOOD ON THIS MAP



Legend

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E

Noise and Overflight Compatibility Factors

- 75 dB CNEL
- 70 dB CNEL
- 65 dB CNEL
- 60 dB CNEL
- 55 dB CNEL Contour Not Shown

Composite of Existing and Future Average Annual Day

Safety and Airspace Compatibility Factors

- Aircraft Departure Accident Risk Intensity Contours* (Shown only for Takeoffs to the Northwest)
- Aircraft Approach Accident Risk Intensity Contours* (Shown only for Landings from the Southeast; shifted 1,500 feet to reflect displaced threshold on primary runway)
- FAR Part 77 Conical Surface Limits
- Terrain Penetration of FAR Part 77 Surfaces

Boundary Lines

- Airport Property Line
- City Limits

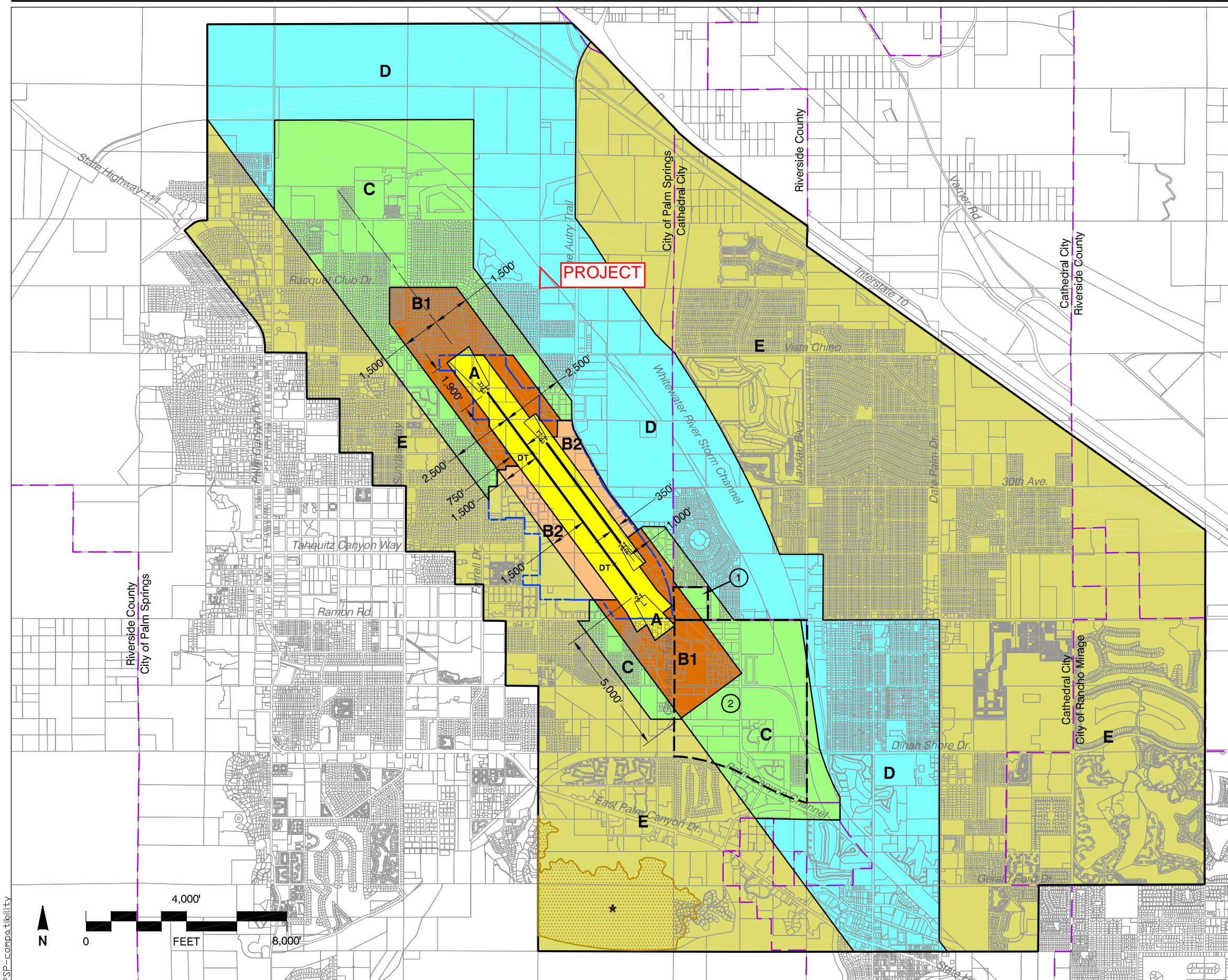
* Aircraft accident risk intensity contours are derived from nationwide accident location data in California Division of Aeronautics database. The contours show relative intensities (highest concentrations) of near-airport accidents in 20% increments. The contour shapes represent a wide range of general aviation airports and have not been modified to reflect the flight tracks for this airport.

**Riverside County
Airport Land Use Commission
Riverside County
Airport Land Use Compatibility Plan
East County Airports Background Data
(March 2005)**

Exhibit PS-7

**Compatibility Factors Map
Palm Springs International Airport**

ESP-compos.tiballty



Legend

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E
- Height Review Overlay Zone

Boundary Lines

- Airport Property Line
- City Limits

Notes

All dimensions measured from runway ends and centerlines.

DT = Displaced Threshold

See Chapter 2, Table 2A for compatibility criteria associated with this map.

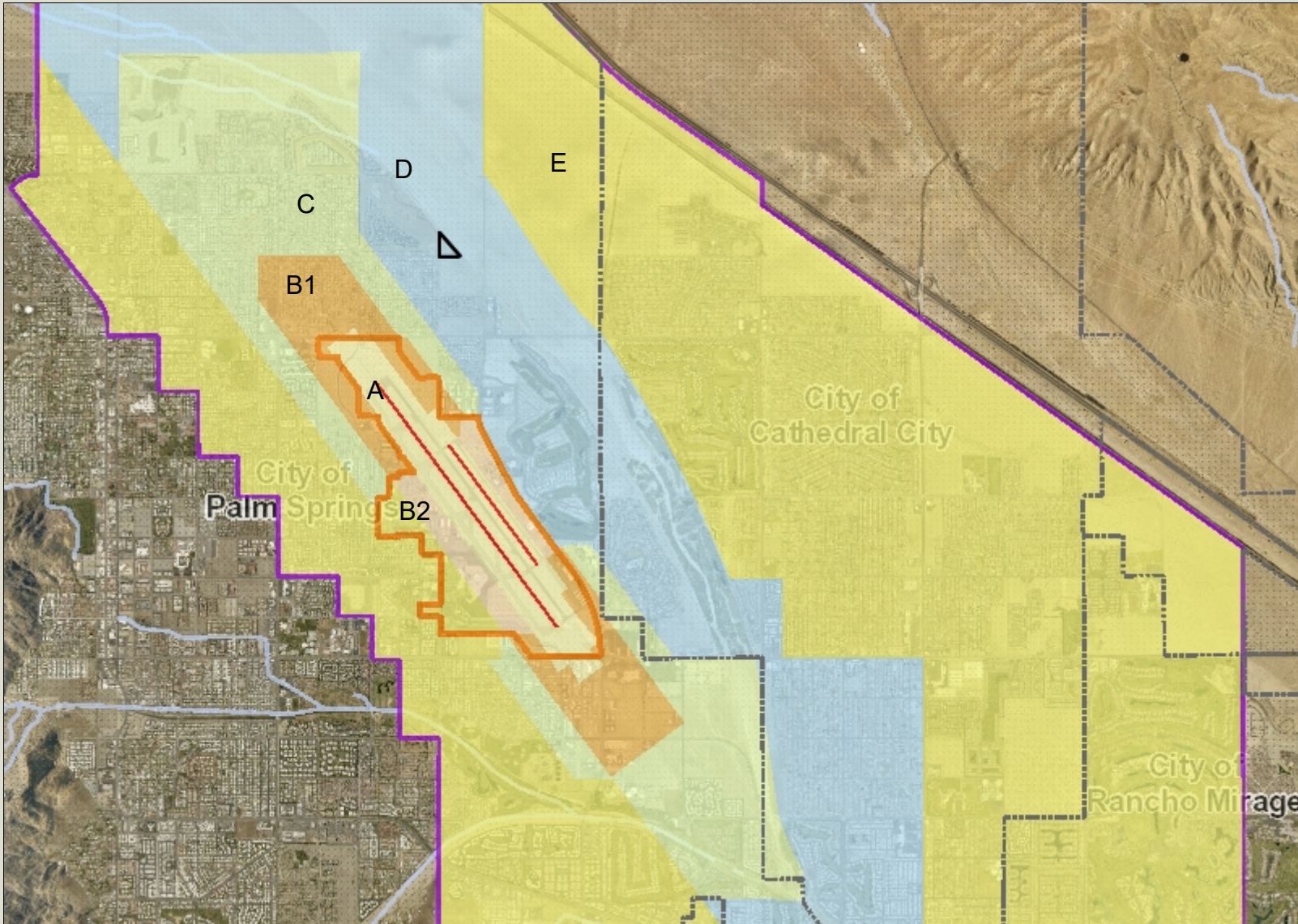
See Policy PS.2.1.

Riverside County
 Airport Land Use Commission
**Riverside County
 Airport Land Use Compatibility Plan
 Policy Document**
 (Adopted March 2005)

Map PS-1

Compatibility Map
 Palm Springs International Airport

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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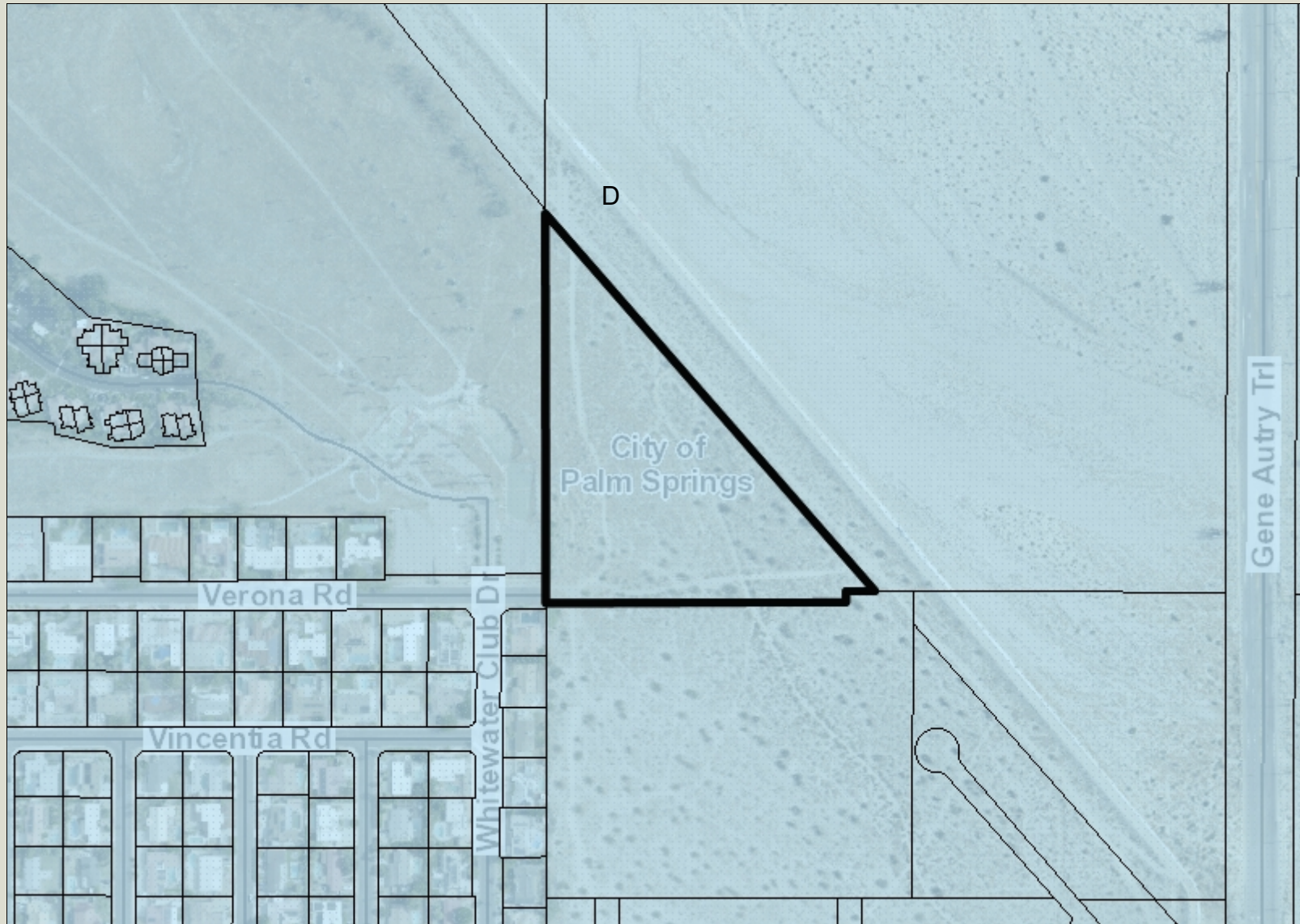


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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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Notes

Map My County Map



Legend

-  Parcels
-  County Centerline Names
-  County Centerlines
-  Blueline Streams
-  City Areas
-  World Street Map



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0 385 770 Feet

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Notes

Map My County Map



Legend

-  Blueline Streams
-  City Areas
-  World Street Map



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Notes



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Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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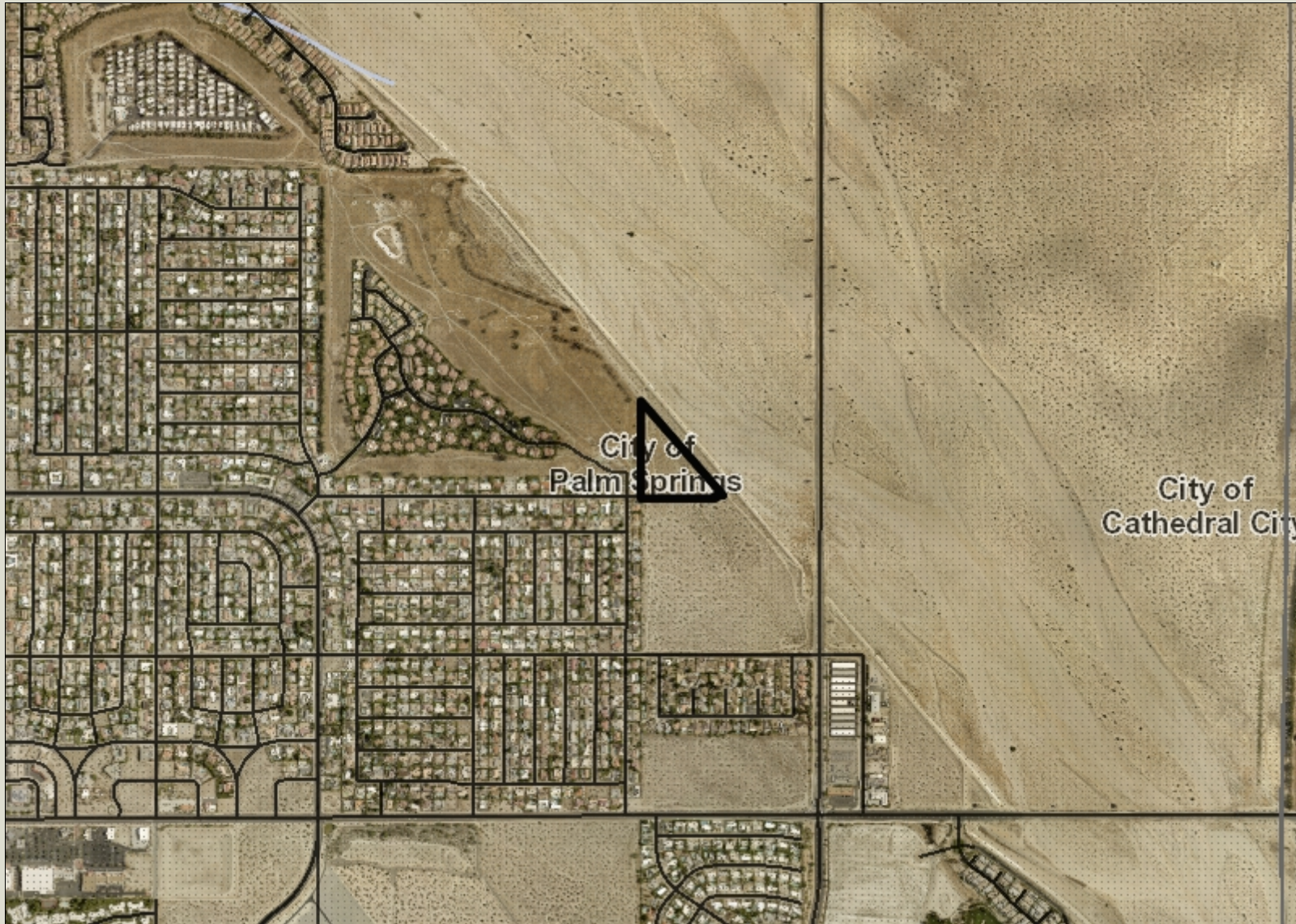
Notes



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Map My County Map



Legend

- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



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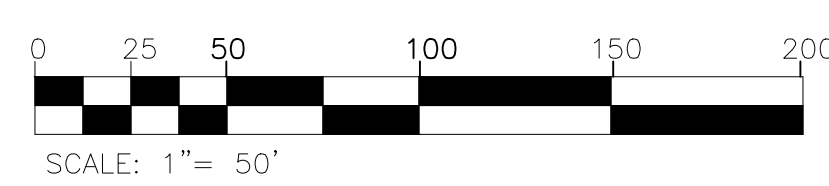


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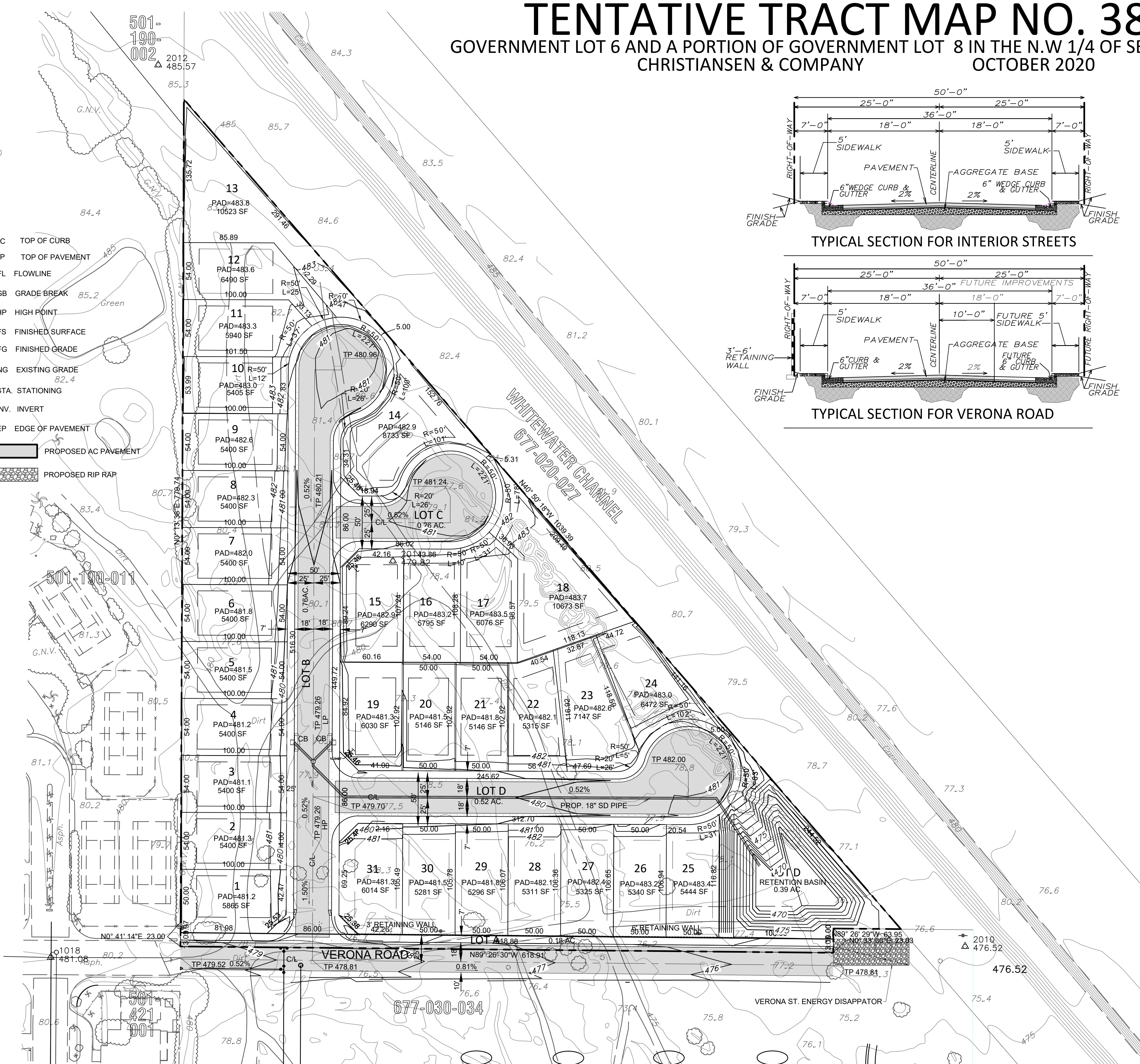
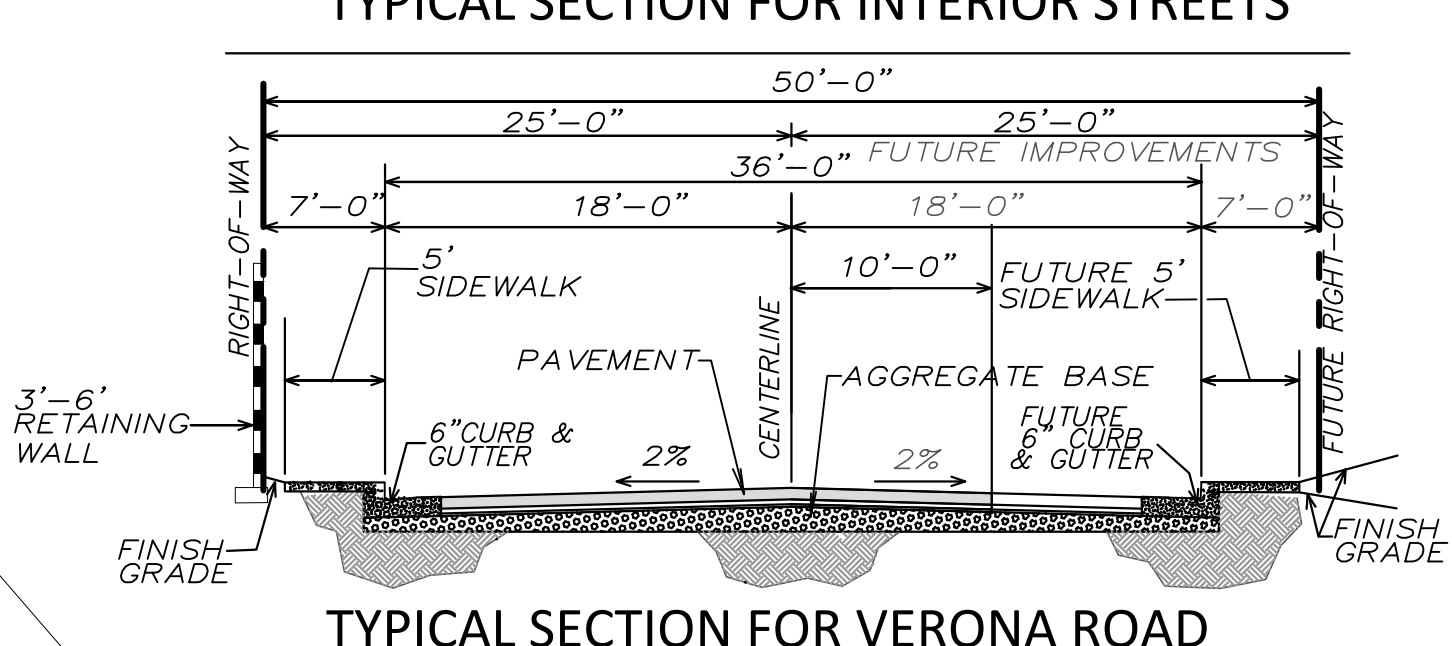
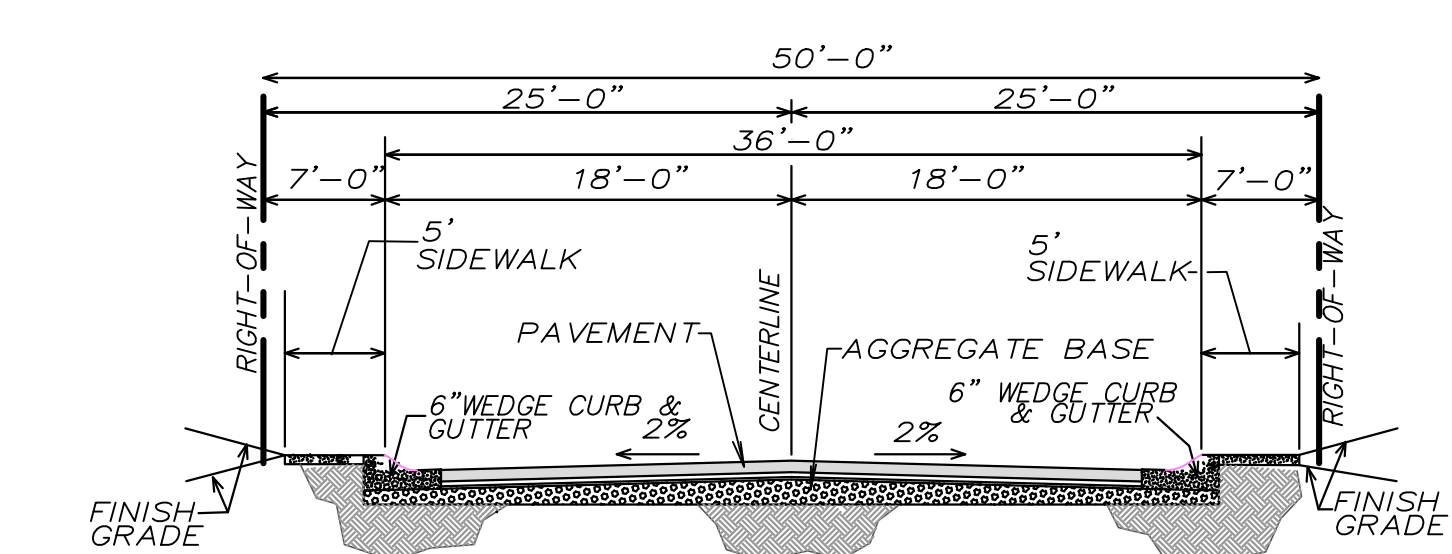
Notes

IN THE CITY OF PALM SPRINGS, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA
TENTATIVE TRACT MAP NO. 38042
 GOVERNMENT LOT 6 AND A PORTION OF GOVERNMENT LOT 8 IN THE N.W 1/4 OF SECTION 6, T.4S., R.5E. S.B.M.
 CHRISTIANSEN & COMPANY
 OCTOBER 2020



LEGEND

- PROPOSED CONTOUR
- EXISTING CONTOUR
- PROPOSED ELEVATION
- EXISTING ELEVATION
- EXISTING TREE/BUSH
- CROSS - GUTTER
- PROPOSED CURB & GUTTER
- EXISTING CURB & GUTTER
- EDGE OF PAVEMENT
- POWER POLE
- EXISTING P.C. CONCRETE
- DIRECTION OF FLOW
- EXISTING WATER SERVICE
- EXISTING SEWER LATERAL
- TC TOP OF CURB
- TP TOP OF PAVEMENT
- FL FLOWLINE
- GB GRADE BREAK
- HP HIGH POINT
- FS FINISHED SURFACE
- FG FINISHED GRADE
- NG EXISTING GRADE
- STA. STATIONING
- INV. INVERT
- EP EDGE OF PAVEMENT
- PROPOSED AC PAVEMENT
- PROPOSED RIP RAP



DATE PREPARED: 10/23/20

OWNER/APPLICANT:
 CASA VERONA, LLC
 699 INDIAN TRAIL
 PALM SPRINGS, CALIFORNIA 92264
 PH: 760-774-3294
 FAX: 858-857-1840
 CONTACT: MARK TEMPLE
 EMAIL: MARK@TEMPLECONSTRUCTIONINC.COM

ENGINEER:
 CHRISTIANSEN & COMPANY
 5225 CANYON CREST DRIVE, STE. 251
 RIVERSIDE, CALIFORNIA 92507
 PH: 951-323-4713
 FAX: 951-405-8410
 CONTACT: KEITH CHRISTIANSEN
 EMAIL: KEITH@CCOCIVIL.COM

ASSESSOR PARCEL NUMBERS:
 677-020-032

MAP SCALE:
 1"=50'

ADDRESS:
 XX-XXX VERONA ROAD - PALM SPRINGS, CALIFORNIA 92264

ACREAGE	LOT	ACREAGE	DEDICATION VERONA ROAD
GROSS: 6.44	A	0.18	INTERIOR STREET LOT
	B	0.76	INTERIOR STREET LOT
	C	0.26	INTERIOR STREET LOT
	D	0.52	RETENTION BASIN
	E	0.39	RESIDENTIAL LOTS
	F-31	4.33	RESIDENTIAL LOTS

ZONING:
 EXISTING: R-1-D
 PROPOSED: R-1-E
 SURROUNDING: RGA8, R-1-D, R-1-C, W

LAND USE/GP:
 EXISTING: VACANT
 PROPOSED: SINGLE FAMILY RESIDENTIAL
 SURROUNDING: RESIDENTIAL, FLOOD CONTROL

UTILITY PURVEYORS:
 WATER: DESERT WATER AGENCY (760)-323-4971
 SEWER: CITY OF PALM SPRINGS (760)-323-8299
 GAS: SOUTHERN CALIFORNIA GAS CO. (760)-341-4523
 ELECTRIC: SOUTHERN CALIFORNIA EDISON (760)-202-4290
 PHONE: FRONTIER COMMUNICATIONS (855)-284-2105
 CABLE: TIME WARNER CABLE (760)-340-1312
 WASTE: BURRTEC WASTE & RECYCLING SERVICES (760)-292-2495

FEMA FLOOD ZONE
 FLOOD ZONE: X PANEL: 06065C1557G EFFECTIVE DATE: 8/28/2008

ELEVATION DATUM:
 +500 FEET HAS BEEN ADDED TO ALL ELEVATION TO ELIMINATE NEGATIVE (-) ELEVATIONS.

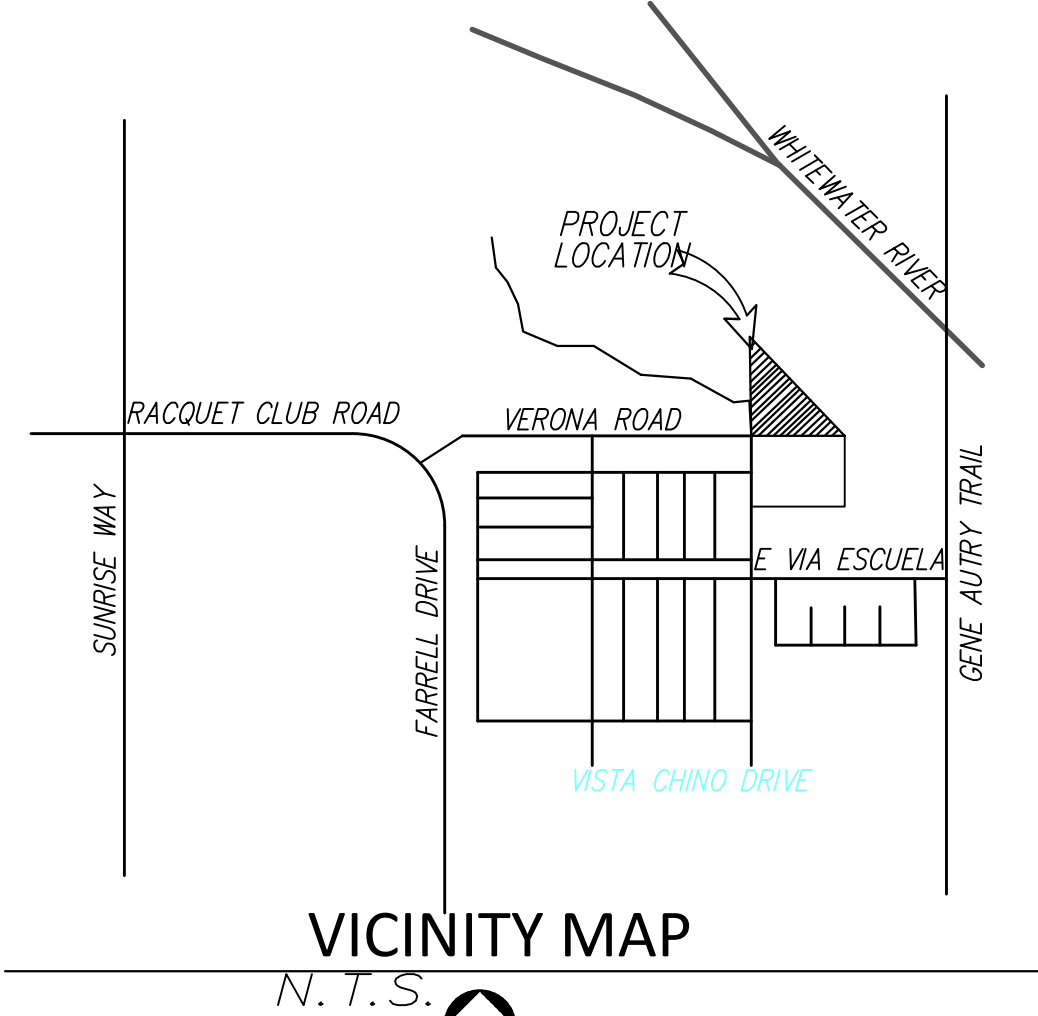
WQMP NOTE:
 AN EARTHEN RETENTION BASIN IS UTILIZED FOR 100% STORMWATER CONTAINMENT OF THE 100YR-24HR STORM EVENT. THEREFORE, WQMP WORKSHEET VOLUMES ARE ACHIEVED.

NOTE:
 PROPERTY NOT IN A COUNTY SERVICE AREA OR COMMUNITY FACILITIES DISTRICT.

ELEVATION DATUM:
 +500 FEET HAS BEEN ADDED TO ALL ELEVATION TO ELIMINATE NEGATIVE (-) ELEVATIONS.

EARTHWORK ESTIMATE

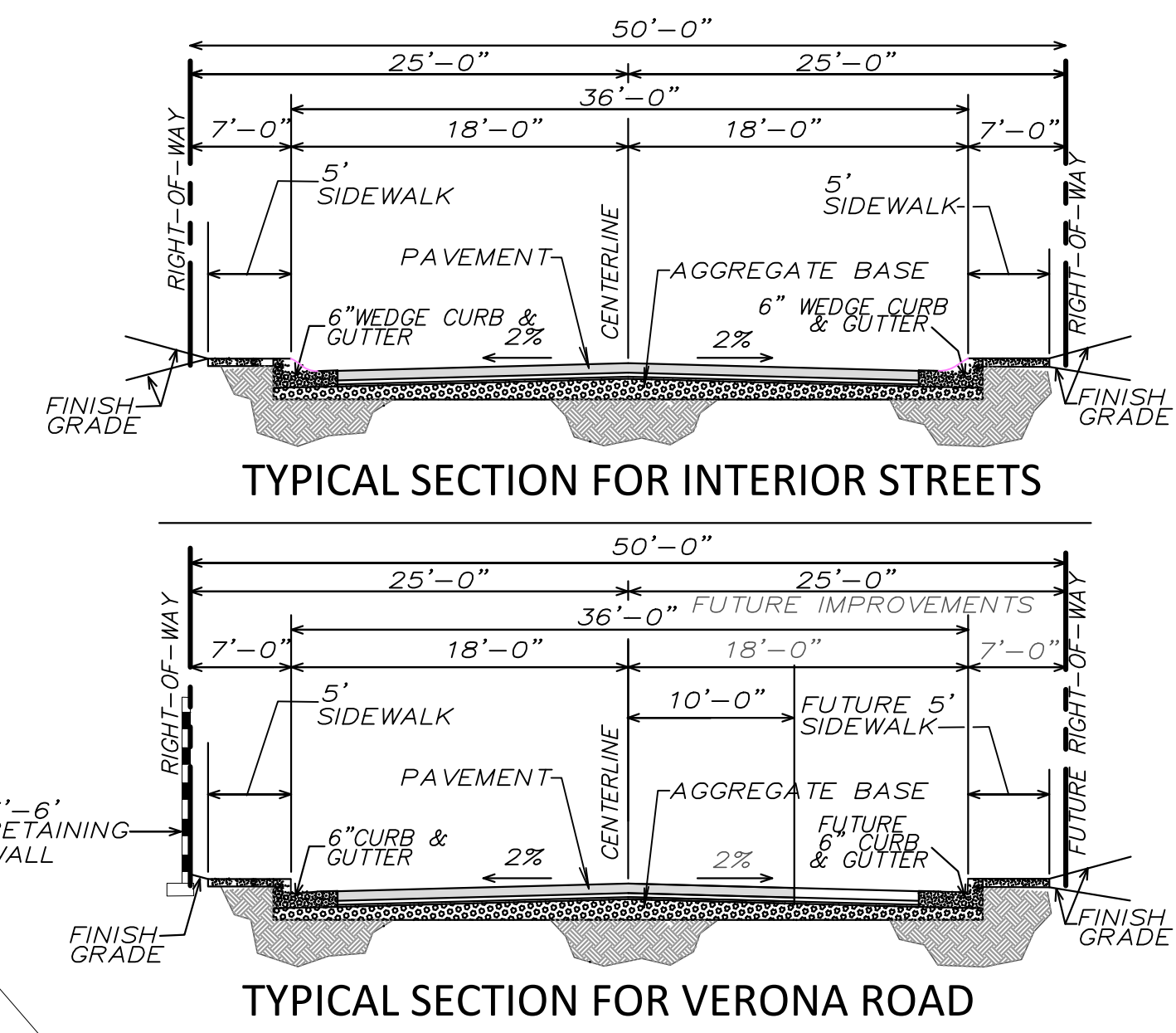
ITEM	CUT (CY)	FILL (CY)
SHRINKAGE (10%)	3200	22000
SUBSIDENCE (0.10')	0	2200
OVEREXCAVATION (2.0')	0	1040
	(750)	0
IMPORT	2450	25240
		22790



NO.	REVISION	DATE	APPROVED	DATE	BENCHMARK	ELEV.	LOCATION	PREPARED UNDER THE DIRECT SUPERVISION OF: KEITH A. CHRISTIANSEN	DESIGN BY: K.A.C.	DRAWN BY: K.A.C.	CHECKED BY: K.A.C.	REVIEWED BY: JOHN M. BRUDIN	APPROVED BY: JOEL MONTALVO	CITY OF PALM SPRINGS, CALIFORNIA TENTATIVE TRACT MAP NO. 38042 APN 677-020-032 CASA VERONA A SUBDIVISION OF A PORTION OF THE NW 1/4 OF SECTION 6, T.4S., R.5E., S.B.B. & M.	FILE NO.	SHEET 1 OF 1 SHEETS
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IN THE CITY OF PALM SPRINGS, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA
TENTATIVE TRACT MAP NO. 38042
 GOVERNMENT LOT 6 AND A PORTION OF GOVERNMENT LOT 8 IN THE N.W 1/4 OF SECTION 6, T.4S., R.5E. S.B.M.
 CHRISTIANSEN & COMPANY
 OCTOBER 2020

Existing: R-1-D
 Proposed: R-1-E



DATE PREPARED: 10/23/20

OWNER/APPLICANT:
 CASA VERONA, LLC
 699 INDIAN TRAIL
 PALM SPRINGS, CALIFORNIA 92264
 PH: 760-774-3294
 FAX: 858-857-1840
 CONTACT: MARK TEMPLE
 EMAIL: MARK@TEMPLECONSTRUCTIONINC.COM

ENGINEER:
 CHRISTIANSEN & COMPANY
 5225 CANYON CREST DRIVE, STE. 251
 RIVERSIDE, CALIFORNIA 92507
 PH: 951-323-4713
 FAX: 951-405-8410
 CONTACT: KEITH CHRISTIANSEN
 EMAIL: KEITH@CCOCIVIL.COM

ASSESSOR PARCEL NUMBERS:
 677-020-032

MAP SCALE:
 1"=50'

ADDRESS:
 XX-XXX VERONA ROAD - PALM SPRINGS, CALIFORNIA 92264

ACREAGE:	LOT	ACREAGE	DEDICATION VERONA ROAD
GROSS: 6.44	A	0.18	INTERIOR STREET LOT
	B	0.76	INTERIOR STREET LOT
	C	0.26	INTERIOR STREET LOT
	D	0.52	RETENTION BASIN
	E	0.39	RETENTION BASIN
	F-31	4.33	RESIDENTIAL LOTS

ZONING:
 EXISTING: R-1-D
 PROPOSED: R-1-E
 SURROUNDING: RGA8, R-1-D, R-1-C, W

LAND USE/GP:
 EXISTING: VACANT
 PROPOSED: SINGLE FAMILY RESIDENTIAL
 SURROUNDING: RESIDENTIAL, FLOOD CONTROL

UTILITY PURVEYORS:

WATER: DESERT WATER AGENCY	(760)-323-4971
SEWER: CITY OF PALM SPRINGS	(760)-323-8299
GAS: SOUTHERN CALIFORNIA GAS CO.	(760)-341-4523
ELECTRIC: SOUTHERN CALIFORNIA EDISON	(760)-202-4290
PHONE: FRONTIER COMMUNICATIONS	(855)-284-2105
CABLE: TIME WARNER CABLE	(760)-340-1312
WASTE: BURRTEC WASTE & RECYCLING SERVICES	(760)-292-2495

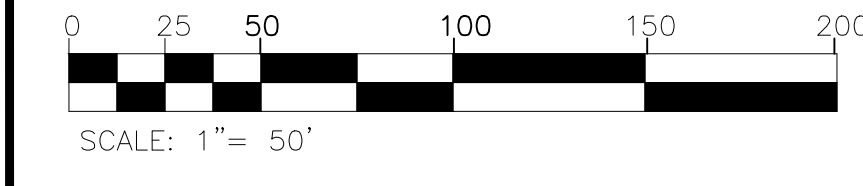
FEMA FLOOD ZONE
 FLOOD ZONE: X PANEL: 06065C1557G EFFECTIVE DATE: 8/28/2008

ELEVATION DATUM:
 +500 FEET HAS BEEN ADDED TO ALL ELEVATION TO ELIMINATE NEGATIVE (-) ELEVATIONS.

WQMP NOTE:
 AN EARTHEN RETENTION BASIN IS UTILIZED FOR 100% STORMWATER CONTAINMENT OF THE 100YR-24HR STORM EVENT. THEREFORE, WQMP WORKSHEET VOLUMES ARE ACHIEVED.

NOTE:
 PROPERTY NOT IN A COUNTY SERVICE AREA OR COMMUNITY FACILITIES DISTRICT.

ELEVATION DATUM:
 +500 FEET HAS BEEN ADDED TO ALL ELEVATION TO ELIMINATE NEGATIVE (-) ELEVATIONS.

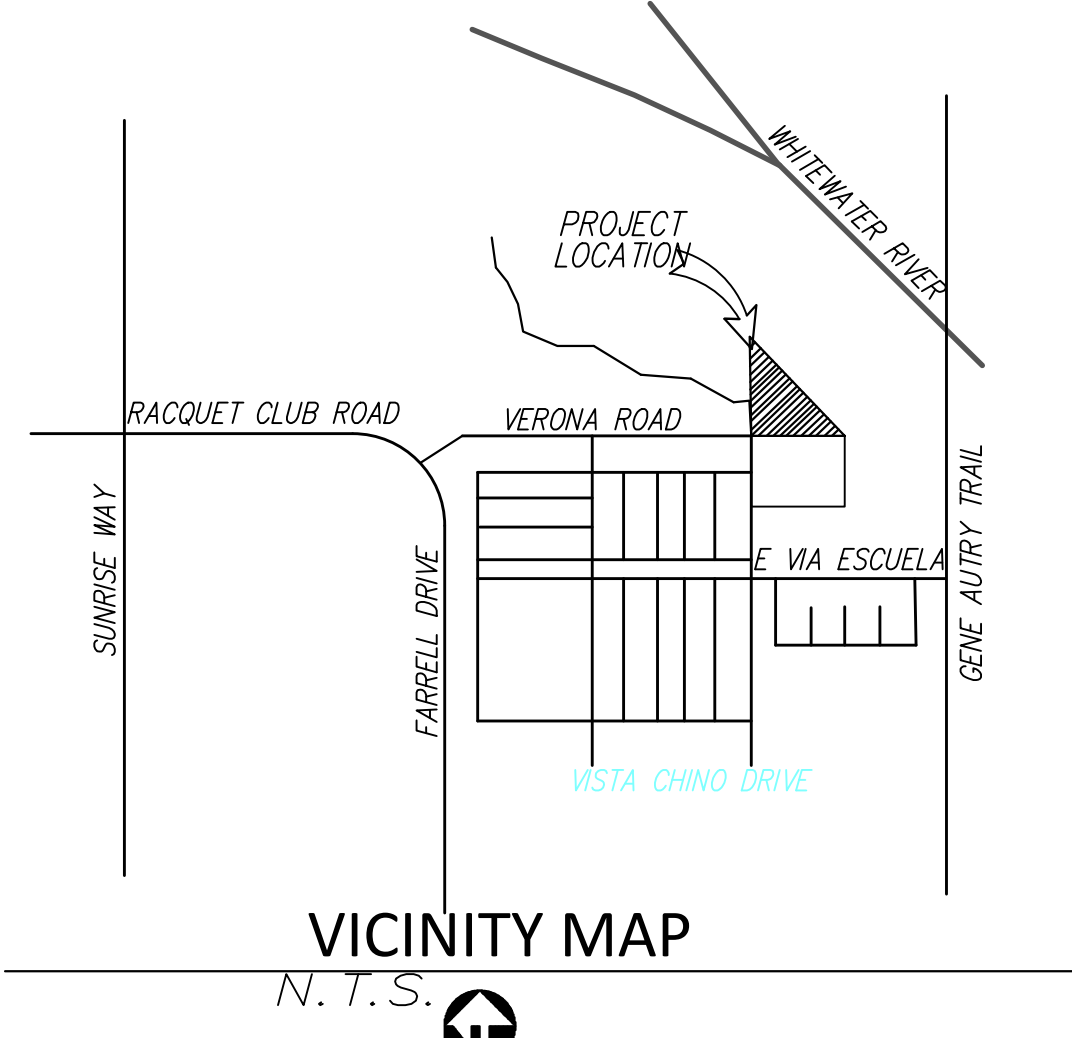
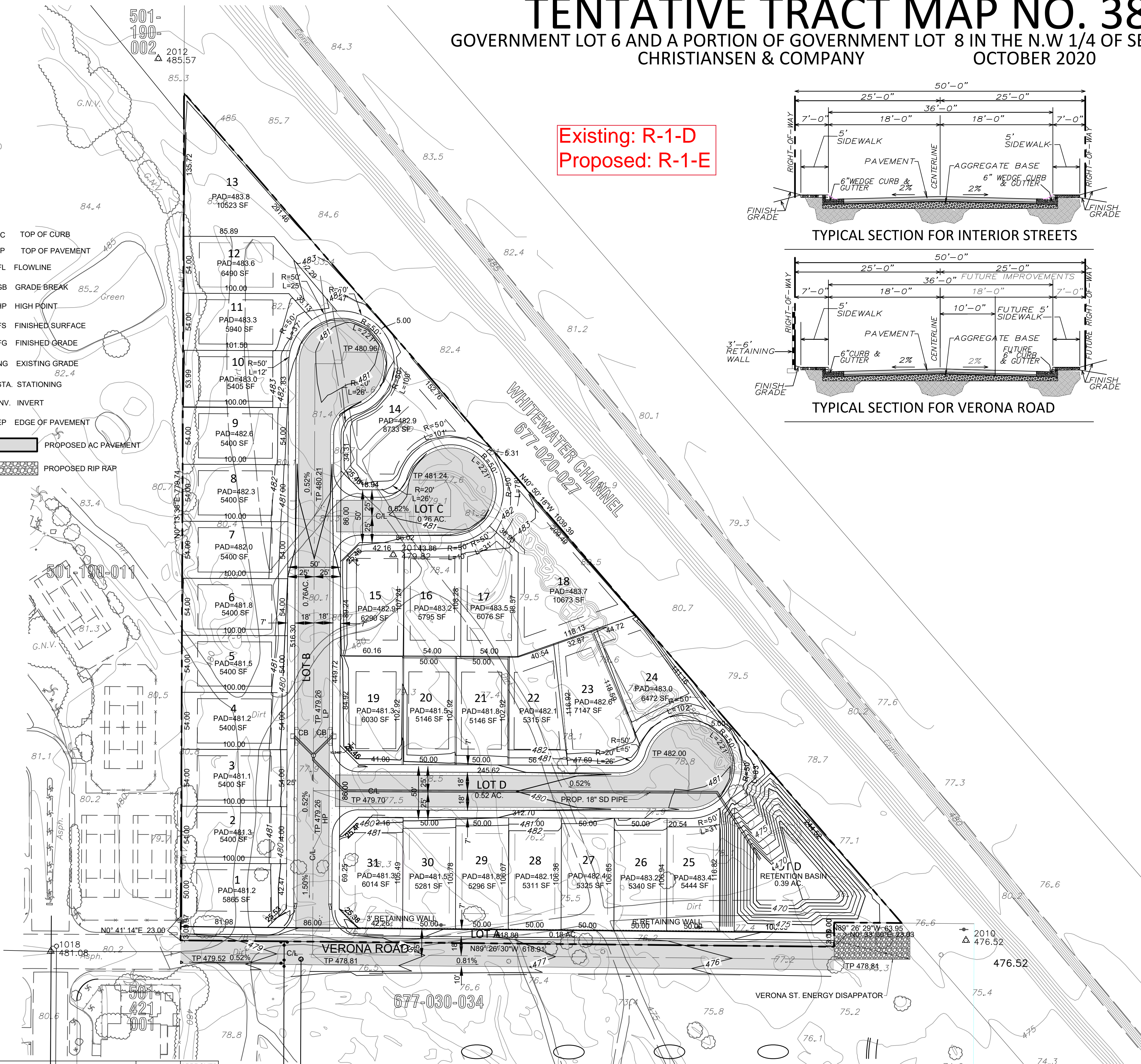


LEGEND

	PROPOSED CONTOUR	TC	TOP OF CURB
	EXISTING CONTOUR	TP	TOP OF PAVEMENT
	PROPOSED ELEVATION	FL	FLOWLINE
	EXISTING ELEVATION	GB	GRADE BREAK
	EXISTING TREE/BUSH	HP	HIGH POINT
	CROSS - GUTTER	FS	FINISHED SURFACE
	PROPOSED CURB & GUTTER	FG	FINISHED GRADE
	EXISTING CURB & GUTTER	NG	EXISTING GRADE
	EDGE OF PAVEMENT	STA.	STATIONING
	POWER POLE	INV.	INVERT
	EXISTING P.C. CONCRETE	EP	EDGE OF PAVEMENT
	DIRECTION OF FLOW		PROPOSED AC PAVEMENT
	EXISTING WATER SERVICE		PROPOSED RIP RAP
	EXISTING SEWER LATERAL		

EARTHWORK ESTIMATE

ITEM	CUT (CY)	FILL (CY)
SHRINKAGE (10%)	3200	22000
SUBSIDENCE (0.10')	0	2200
OVEREXCAVATION (2.0')	0	1040
	(750)	0
	2450	25240
IMPORT		22790



NO.	REVISION	APPROVED	DATE	BENCHMARK	ELEV.	PREPARED UNDER THE DIRECT SUPERVISION OF:	DESIGN BY:	REVIEWED BY:	APPROVED BY:	CITY OF PALM SPRINGS, CALIFORNIA	FILE NO.	SHEET
						KEITH A. CHRISTIANSEN	K.A.C.	JOHN M. BRUDIN	JOEL MONTALVO	TENTATIVE TRACT MAP NO. 38042		1
										APN 677-020-032		
										CASA VERONA		
										A SUBDIVISION OF A PORTION OF THE N.W 1/4 OF SECTION 6, T.4S., R.5E., S.B.B. & M.		OF 1 SHEETS

NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. **Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org.** The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893.**

The City of Palm Springs Planning Department should be contacted on non-ALUC issues. For more information please contact City of Palm Springs Planner Glenn Mlaker at (760) 323-8245.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to prull@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please contact Barbara Santos at (951) 955-5132.

PLACE OF HEARING: **Riverside County Administration Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside California**

DATE OF HEARING: **July 14, 2022**

TIME OF HEARING: **9:30 A.M.**

CASE DESCRIPTION:

ZAP1101PS22 – Casa Verona, LLC (Representative: Temple Construction Inc.) – City of Palm Springs Case Nos. 5.1527 (Change of Zone), TTM38042 (Tentative Tract Map). A proposal to divide 6.44 gross acres into 31 single-family residential lots and one retention basin lot, located northerly of Verona Road, easterly of Whitewater Club Drive, and westerly of the Whitewater Channel. The applicant also proposes changing the site's zoning from R-1-D (Single Family Residential minimum 7,500 square foot lot size) to R-1-E (Small-lot single-family residential zone) (Airport Compatibility Zone D of the Palm Springs International Airport Influence Area)

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM: 3.3

HEARING DATE: July 14, 2022

CASE NUMBER: ZAP1517MA22 – BCI IV Harvill Industrial Center
(Representative: EPD Solutions)

APPROVING JURISDICTION: County of Riverside

JURISDICTION CASE NO: PPT220001 (Plot Plan)

LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use
Compatibility Plan

Airport Influence Area: March Air Reserve Base

Land Use Policy: Zones C2

Noise Levels: Below 60 CNEL contour

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends that the Commission find the proposed Plot Plan **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, subject to the conditions included herein.

PROJECT DESCRIPTION: A proposal to construct a 99,770 square foot manufacturing building on 9.13 acres. The applicant also proposes to construct a rooftop solar panel system on the proposed building

PROJECT LOCATION: The site is located northerly of Cajalco Road, westerly of Interstate 215, easterly of Harvill Avenue, and southerly of Dree Circle, approximately 8,948 feet southwesterly of the southerly end of Runway 14-32 at March Air Reserve Base.

BACKGROUND:

Non-Residential Average Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zone C2, where Zone C2 limits average intensity to 200 people per acre.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan and the Additional Compatibility Policies included in the March ALUCP, the following rates were used to calculate the occupancy for the proposed project:

- Manufacturing – 1 person per 200 square feet, and

- Office – 1 person per 200 square feet.

The project proposes to construct a 99,770 square foot manufacturing building, which includes 96,270 square feet of manufacturing area, and 3,500 square feet of office area, accommodating a total occupancy of 499 people, resulting in an average intensity of 55 people per acre, which is consistent with Zone C2 average intensity criterion of 200 people per acre.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle and 1.0 persons per trailer truck space). Based on the number of parking spaces provided (73 standard vehicles, 118 trailer spaces), the total occupancy would be estimated at 228 people for an average intensity of 25 people per acre, which is consistent with the Compatibility Zone C2 average intensity criterion.

Non-Residential Single-Acre Intensity: Compatibility Zone C2 limits maximum single-acre intensity to 500 people. There are no risk-reduction design bonuses available, as March Air Reserve Base/Inland Port Airport is primarily utilized by large aircraft weighing more than 12,500 pounds.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre area would include 40,060 square feet of manufacturing area, and 3,500 square of office area, resulting in a single acre occupancy of 218 people which is consistent with the Compatibility Zone C2 single acre criterion of 500.

March Air Reserve Base/United States Air Force Input: Given that the project site is located in Zone C2 westerly of the southerly runway at March Air Reserve Base, the March Air Reserve Base staff was notified of the proposal of rooftop solar panels, and sent a solar glare hazard analysis study for their review. On June 3, 2022, the Air Force provided comments concurring with the analysis and conclusions of the glare study, with the added note that their general concerns regarding cumulative impacts of solar projects are being addressed in the ongoing March Compatibility Use Study update.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zones C2 (children's schools, day care centers, hospitals, nursing homes, libraries, places of assembly, highly noise-sensitive outdoor nonresidential uses and hazards to flight).

Flight Hazard Issues: Structure height, electrical interference, and reflectivity/glare are among the issues that solar panels in the airport influence area must address. The project's 99,800 square foot photovoltaic (PV) panel structures would be located on the rooftop of the existing industrial building within Compatibility Zone C2.

Glint and Glare/Reflectivity

Based on the Federal Aviation Administration's Interim Policy for Review of Solar Energy System Projects on Federally Obligated Airports, no glare potential or low potential for temporary after-image ("green" level) are acceptable levels of glare on final approach (within 2 miles from end of runway) for solar facilities located on airport property. However, potential for temporary after-image ("yellow" level) and potential for permanent eye damage ("red" level) are not acceptable levels of glare on final approach. No glare is permitted at air traffic control towers.

The project proposes 99,770 square feet of solar panels on the proposed building rooftop with a fixed tilt of 33 degrees with no rotation, and an orientation of 160 degrees. The applicant has submitted a glare analysis utilizing the web-based Forge Solar, a copy of which is attached hereto. The analysis was based on a 2 mile straight in approach (as per FAA Interim Policy standards) to runways 14 and 32, and also based on the traffic patterns as identified by March Air Reserve Base staff (Runway 12/30 General Aviation, Runway 14/32 General Aviation, Runway 14/32 C-17/KC-135, Runway 14/32 Overhead). The analysis utilized a glide slope approach of 3.0 degrees. No glare would affect the Air Traffic Control Tower.

The analysis concluded that no glare would occur on the 2 mile approach to the runways, and some potential for glare was identified within the Air Force traffic pattern. Evaluation of the Air Force traffic patterns indicates that the panels would result in a low potential for temporary after-image ("green" level glare) or no glare. All times are in standard time.

- Runway 14/32 C-17/KC-135 traffic pattern totaling 621 minutes of 'green' level glare, lasting up to 30 minutes a day, in April and in September from 3:00 p.m. to 4:00 p.m.
- Runway 14/32 Overhead Aviation traffic pattern totaling 5,922 minutes of 'green' level glare, lasting up to 30 minutes a day, between March to October from 2:00 p.m. to 4:00 p.m.

The total of 6,543 minutes of "green" level glare represents less than 3 percent of total day light time.

Electrical and Communication Interference

The applicant has indicated that they do not plan to utilize equipment that would interfere with aircraft communications. The PV panels themselves present little risk of interfering with radar transmission due to their low profiles. In addition, solar panels do not emit electromagnetic waves over distances that could interfere with radar signal transmissions, and any electrical facilities that do carry concentrated current will be buried beneath the ground and away from any signal transmission. There are no radar transmission or receiving facilities within the site.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being below the 60 CNEL range from aircraft noise. Therefore, no special measures are required to mitigate aircraft-generated noise.

Part 77: The elevation of Runway 14-32 at its southerly terminus is 1,488 feet above mean sea level (AMSL). At a distance of approximately 8,948 feet from the project to the nearest point on the runway, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,577 feet AMSL. The site's finished floor elevation is 1,508 feet AMSL and proposed building height is 41 feet, resulting in a top point elevation of 1,549 feet AMSL. Therefore, review of the building for height/elevation reasons by the FAA Obstruction Evaluation Service (FAAOES) is not required.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33C). The project is located 8,948 feet from the runway, and therefore would be subject to the above requirement.

The project includes a 28,250 square foot bioretention basin. Bioretention basins are not recommended in the vicinity of airports due to the potential that such areas could provide food, water, and shelter for hazardous wildlife. Pursuant to the study "Wildlife Hazard Management at Riverside County Airports: Background and Policy", October 2018, by Mead & Hunt, which is the basis of the brochure titled "Airports, Wildlife and Stormwater Management", such limited basins are permissible with the appropriate criteria: the basin is used in conjunction with appropriate landscaping for such uses as adjacent to structures, parking islands, medians, site entrances, planter boxes, and vegetation is selected carefully so as not to provide food, shelter, nesting, roosting, or water for wildlife. The project has been conditioned to be consistent with the basin criteria (as well as providing 48-hour draw down of the basin).

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight or circling climb following takeoff or toward an aircraft engaged in a straight or circling final approach toward a landing at an airport, other than a DoD or FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight or circling climb following takeoff or towards an aircraft engaged in a straight or circling final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.

- (e) Highly noise-sensitive outdoor nonresidential uses. Examples of noise-sensitive outdoor nonresidential uses that are prohibited include, but are not limited to, major spectator-oriented sports stadiums, amphitheaters, concert halls and drive-in theaters.
 - (f) Other Hazards to flight.
3. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property, and be recorded as a deed notice.
 4. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

5. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
6. The project has been evaluated to construct 99,770 square foot manufacturing building, which includes 96,270 square feet of warehouse area, and 3,500 square feet of office area. Any increase in building area, change in use to any higher intensity use, change in building location, or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.
7. All solar arrays installed on the project site shall consist of smooth glass photovoltaic solar panels without anti-reflective coating, a fixed tilt of 33 degrees and orientation of 160 degrees. Solar panels shall be limited to a total of 99,770 square feet, and the locations and

coordinates shall be as specified in the glare study. Any deviation from these specifications (other than reduction in square footage of panels), including change in orientation, shall require a new solar glare analysis to ensure that the amended project does not result in any glare impacting the air traffic control tower or creation of any "yellow" or "red" level glare in the flight paths, and shall require a new hearing by the Airport Land Use Commission.

8. In the event that any glint, glare, or flash affecting the safety of air navigation occurs as a result of project operation, upon notification to the airport operator of an event, the airport operator shall notify the project operator in writing. Within 30 days of written notice, the project operator shall be required to promptly take all measures necessary to eliminate such glint, glare, or flash. An "event" includes any situation that results in an accident, incident, "near-miss," or specific safety complaint regarding an in-flight experience to the airport operator or to federal, state, or county authorities responsible for the safety of air navigation. The project operator shall work with the airport operator to prevent recurrence of the incidence. Suggested measures may include, but are not limited to, changing the orientation and/or tilt of the source, covering the source at the time of day when events of glare occur, or wholly removing the source to diminish or eliminate the source of the glint, glare, or flash. For each such event made known to the project operator, the necessary remediation shall only be considered to have been fulfilled when the airport operator states in writing that the situation has been remediated to the airport operator's satisfaction.
9. In the event that any electrical interference affecting the safety of air navigation occurs as a result of project operation, upon notification to the airport operator of an event, the airport operator shall notify the project operator in writing. Within 30 days of written notice, the project operator shall be required to promptly take all measures necessary to eliminate such interference. An "event" includes any situation that results in an accident, incident, "near-miss," report by airport personnel, or specific safety complaint to the airport operator or to federal, state, or county authorities responsible for the safety of air navigation. The project operator shall work with the airport operator to prevent recurrence of the event. For each such event made known to the project operator, the necessary remediation shall only be considered to have been fulfilled when the airport operator states in writing that the situation has been remediated to the airport operator's satisfaction.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

**THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS**

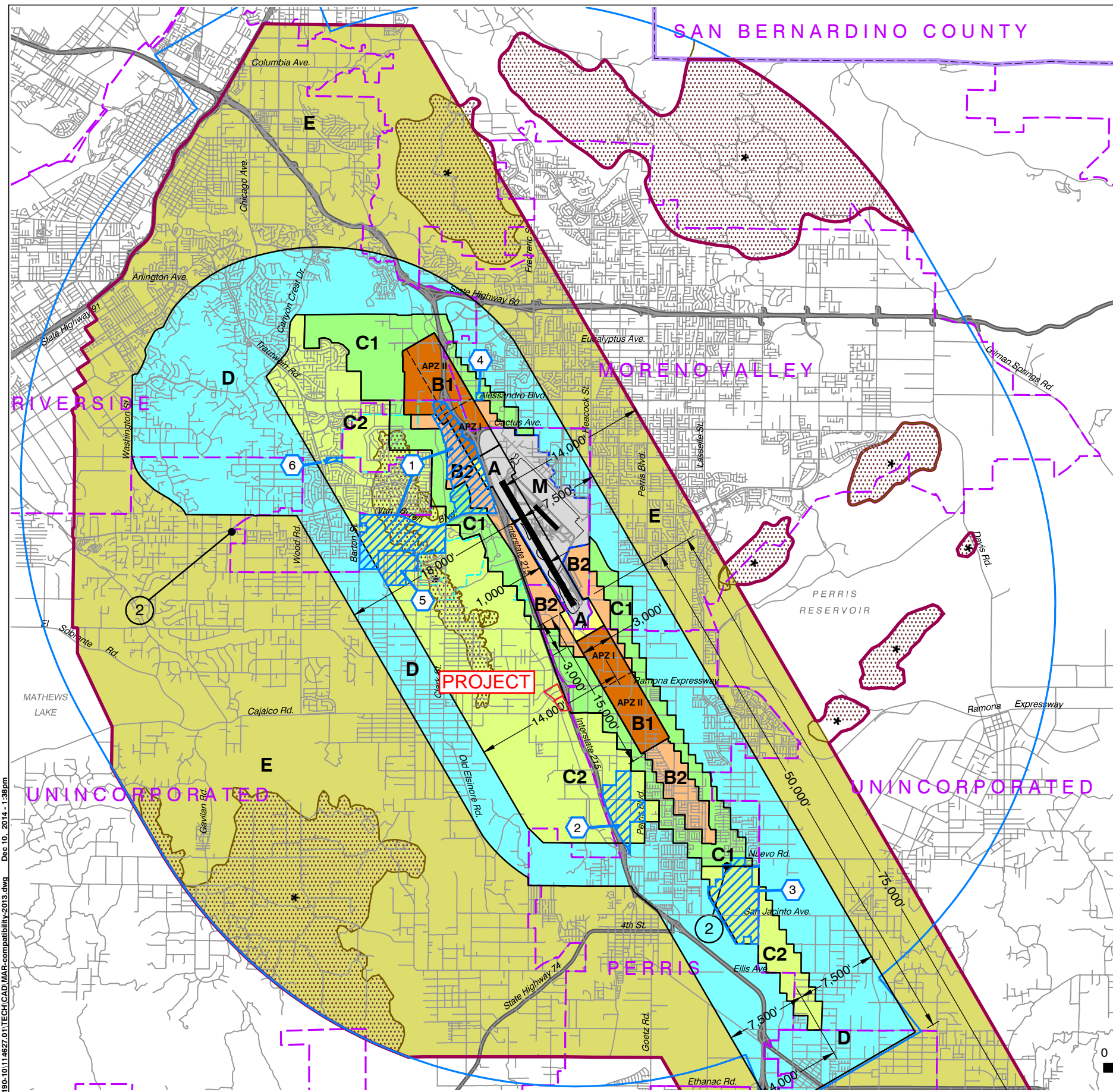
**PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES**



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



LEGEND

Compatibility Zones

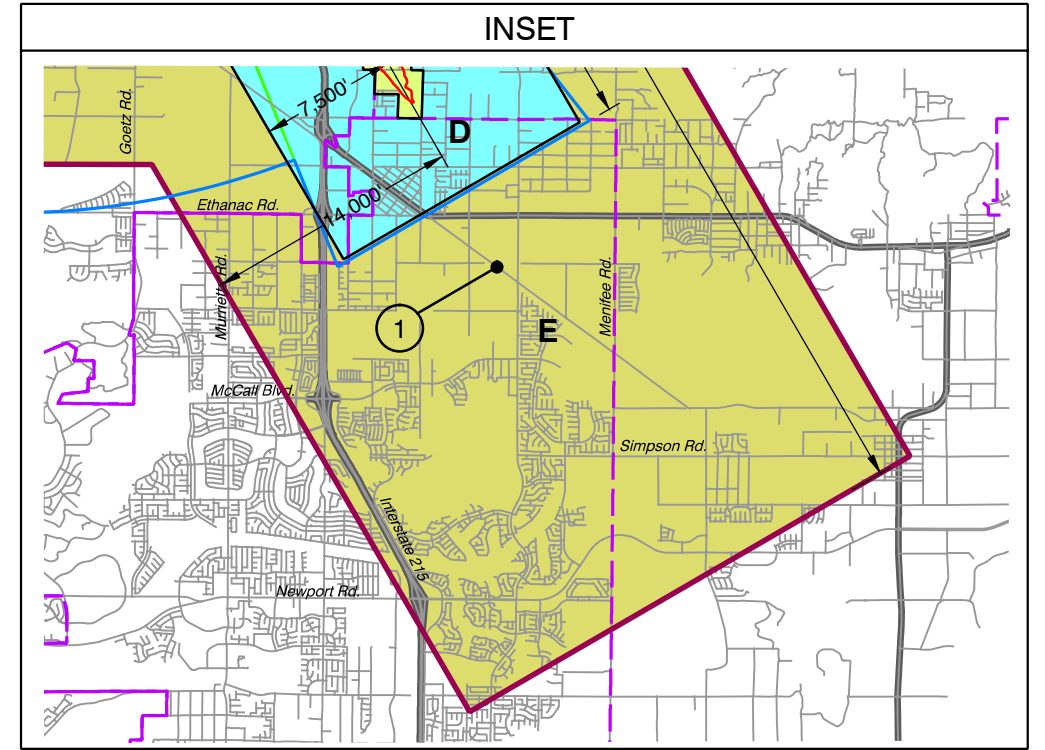
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



**Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)**

Note:
All dimensions are measured from runway ends and centerlines.



Base map source: County of Riverside 2013

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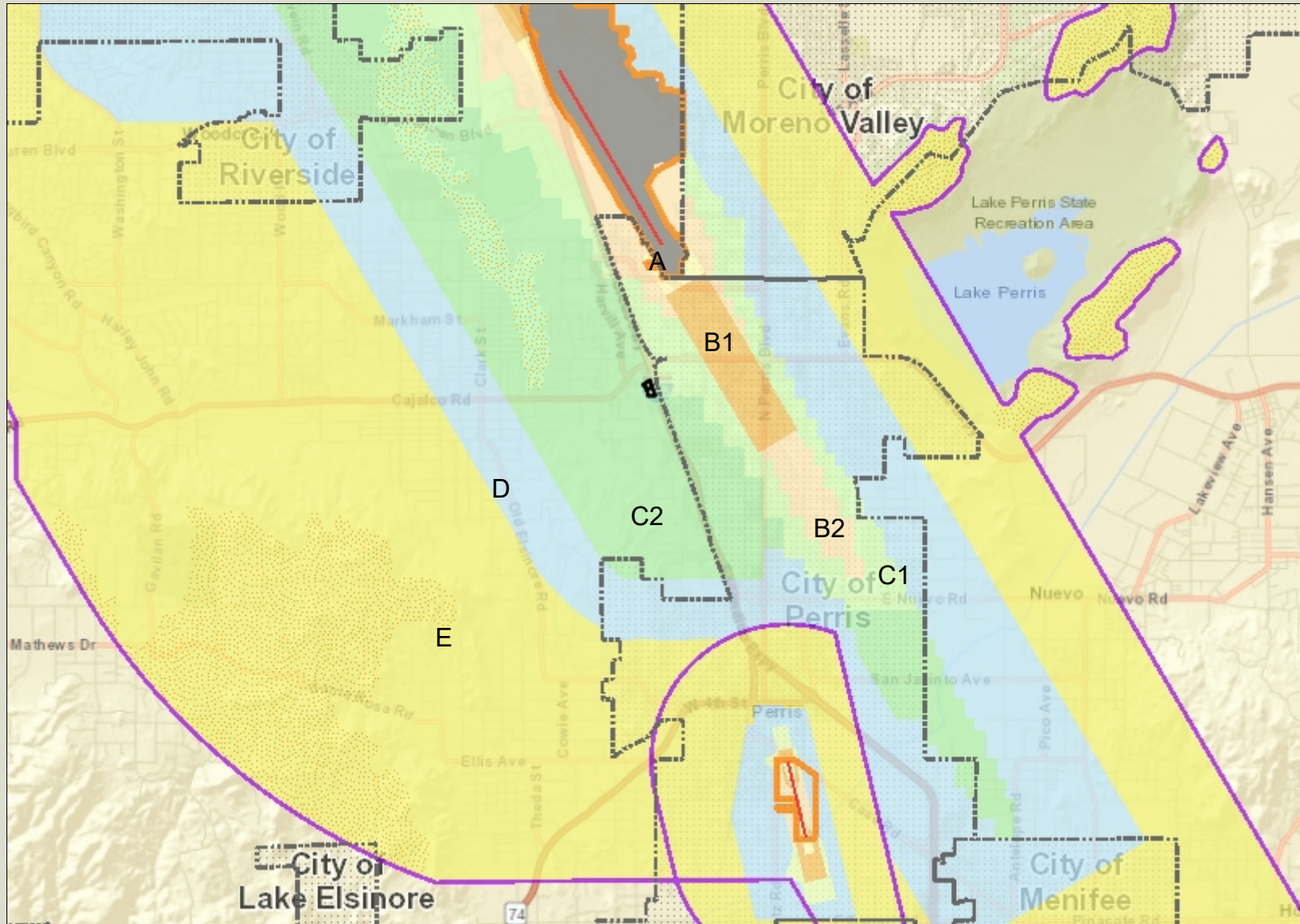
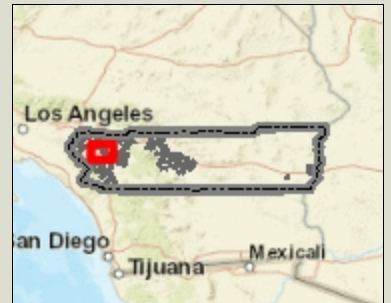
Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map MA-1

**Compatibility Map
March Air Reserve Base / Inland Port Airport**

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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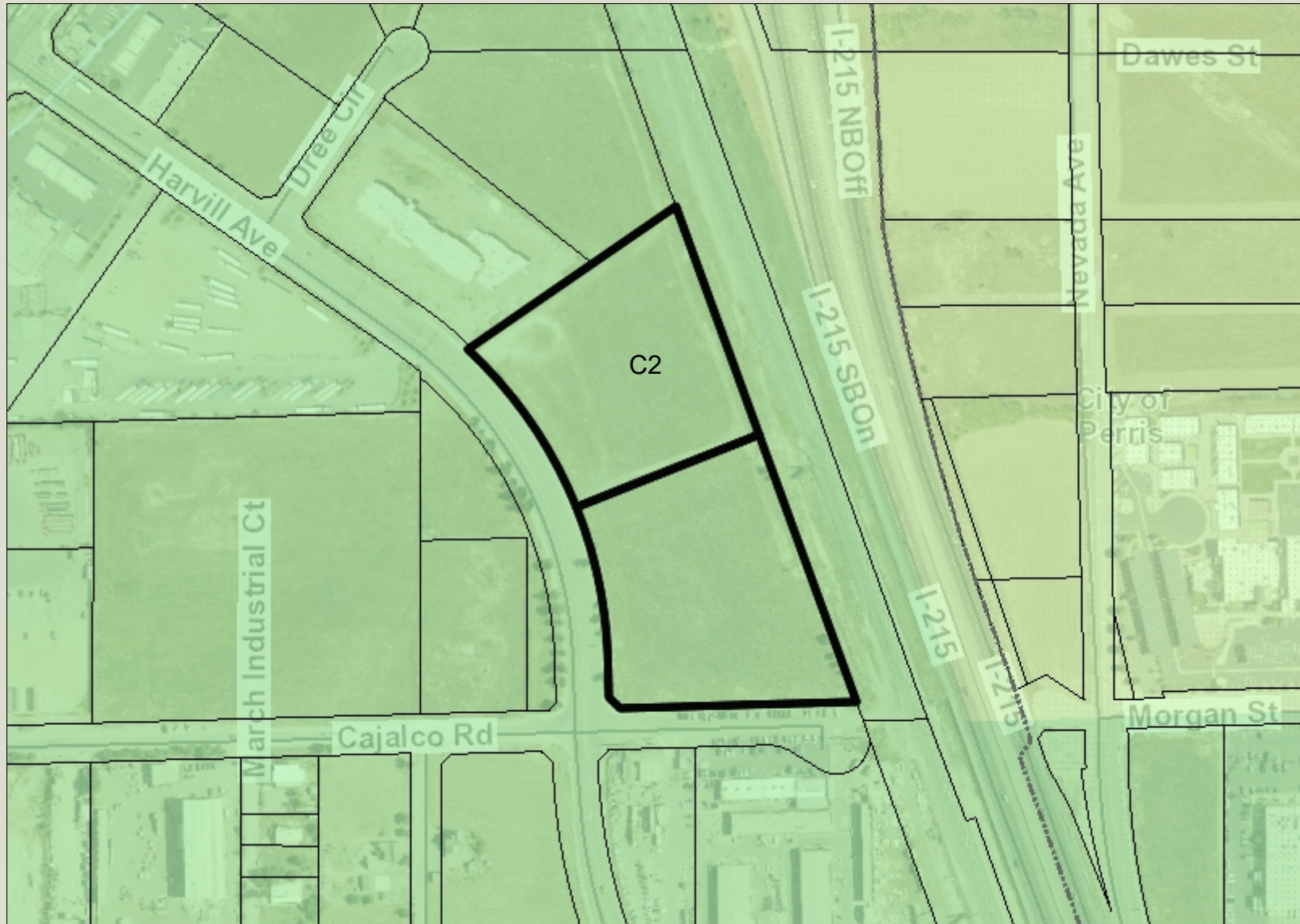


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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas

Airport Compatibility Zones

- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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Notes

Map My County Map



Legend

- Parcels
- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



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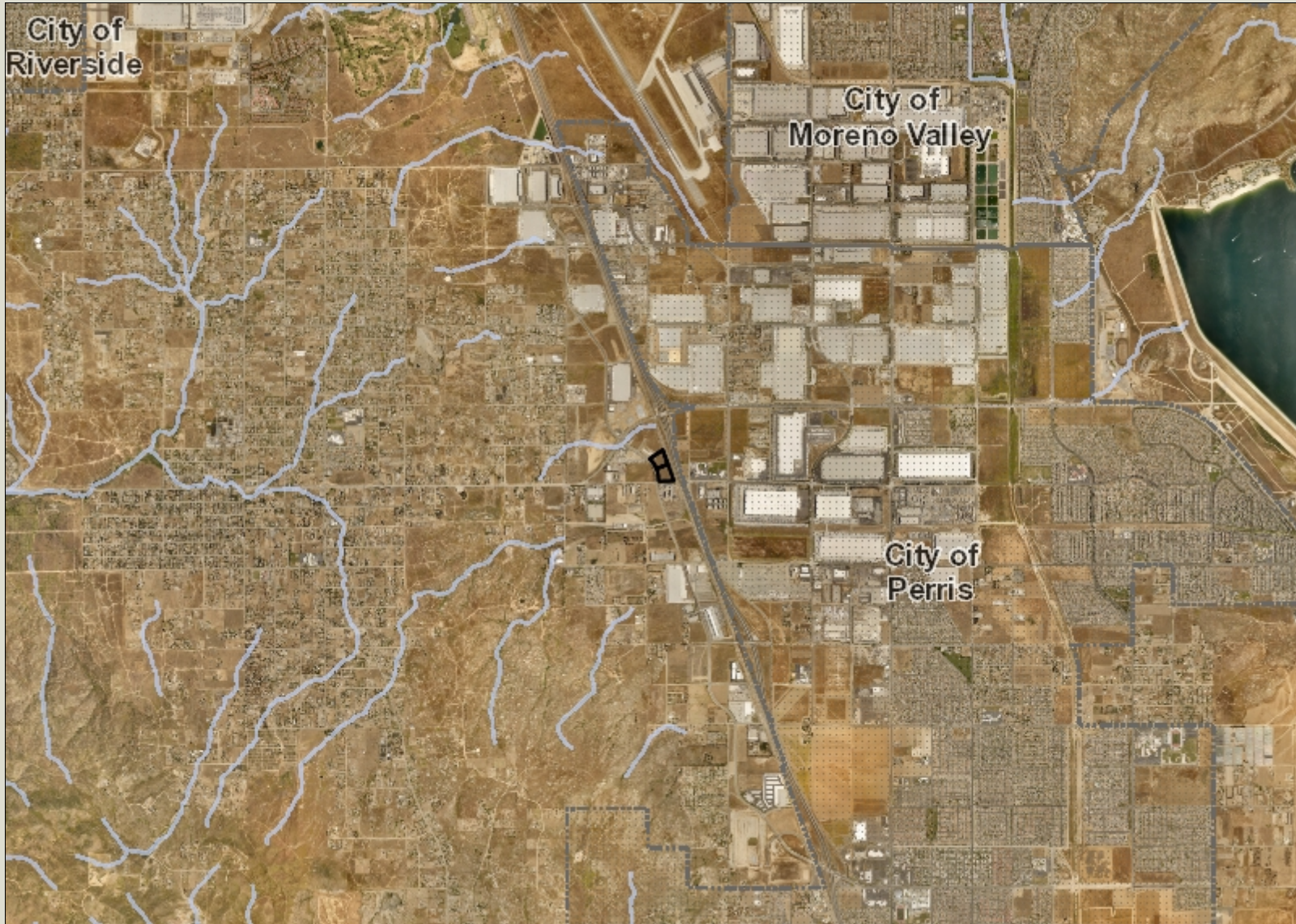
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Notes

Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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Notes



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Legend

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Notes



Map My County Map



- Legend**
- County Centerline Names
 - County Centerlines
 - Blueline Streams
 - City Areas
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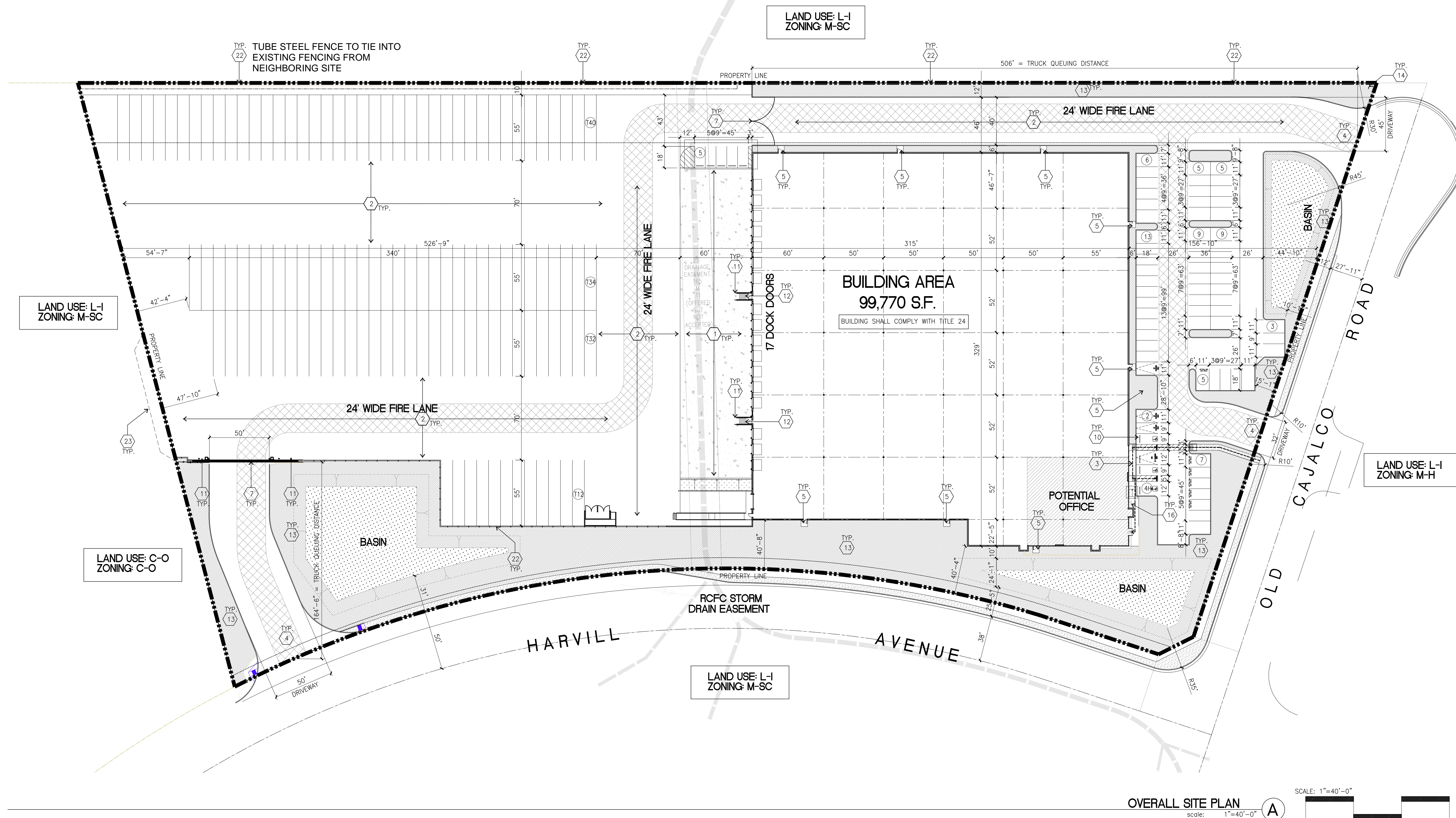
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.



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Notes



Tabulation

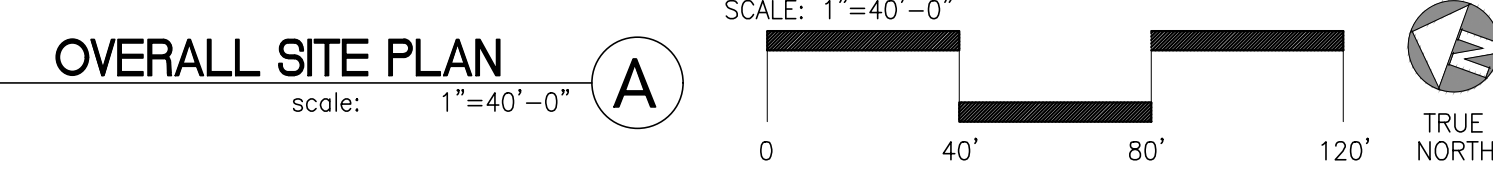
SITE AREA	
In s.f.	397,518 s.f.
In acres	9.13 ac
BUILDING AREA (Foot print: 99,770 sf)	
Office	3,500 s.f.
Warehouse	96,270 s.f.
TOTAL	99,770 s.f.
COVERAGE	
AUTO PARKING REQUIRED	
Office: 1/250 s.f.	14 stalls
Whse: 1/2,000 s.f.	48 stalls
TOTAL	62 stalls
AUTO PARKING PROVIDED	
Standard (9' x 18')	60
Accessible Parking (9' x 18')	2
Accessible Van Parking (12' x 18')	1
EV Standard Parking (9' x 18')	3
EV Standard Accessible (9' x 18')	0
EV Van Accessible (12' x 18')	1
Clean Air Vehicle (9' x 18')	6
Total	73
TRAILER PARKING PROVIDED	
Trailer (10' x 55')	118 stalls
ZONING ORDINANCE FOR COUNTY	
Zoning Designation - Manufacturing - Service commercial (M-SC)	
MAXIMUM BUILDING HEIGHT ALLOWED	
Height: 42'-6" (40'-0" from property line min.)	
MAXIMUM FLOOR AREA RATIO	
FAR - to be verified	
LANDSCAPE REQUIREMENT	
Percentage - 10 %	
LANDSCAPE PROVIDED	
In percentage -	14.6%
In s.f.	57,883 s.f.
SETBACKS	
Building	Parking/Landscape
Front - 25', 25' if abuts R zone	10'
Side/Rear - 0'	0

LEGAL DESCRIPTION

BEING A PORTION OF THE NORTHWEST ONE-QUARTER OF SECTION 12, TOWNSHIP 4 SOUTH, RANGE 4 WEST, S.B.M., MORE PARTICULARLY DESCRIBED AS PARCEL 34 & 35 OF PARCEL MAP 24110, RECORDED IN BOOK 165 OF PARCEL MAPS PAGES 11-18 IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA.

NOTE:

- PROJECT SITE IS LOCATED WITHIN THE COMMUNITY DEVELOPMENT: LIGHT INDUSTRIAL (CD: LI) LAND USE DESIGNATION.
- PROJECT SITE IS LOCATED IN AN AREA OF HIGH PALEONTOLOGICAL SENSITIVITY.



PROPERTY OWNERS AND APNS

- BCI IV HARVILL INDUSTRIAL CENTER LP
- BEING A PORTION OF THE NORTHWEST ONE-QUARTER OF SECTION 12, TOWNSHIP 4 SOUTH, RANGE 4 WEST, S.B.M., MORE PARTICULARLY DESCRIBED AS PARCEL 34 & 35 OF PARCEL MAP 24110, RECORDED IN BOOK 165 OF PARCEL MAPS PAGES 11-18 IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA.

ZONING

MANUFACTURING - SERVICE COMMERCIAL (M-SC)

APPLICANT'S REPRESENTATIVE

HPA, INC.
18831 BARDEEN AVE., STE 100
IRVINE, CA 92612
PHONE: (949) 863-1770
CONTACT: ANDY WYANTO

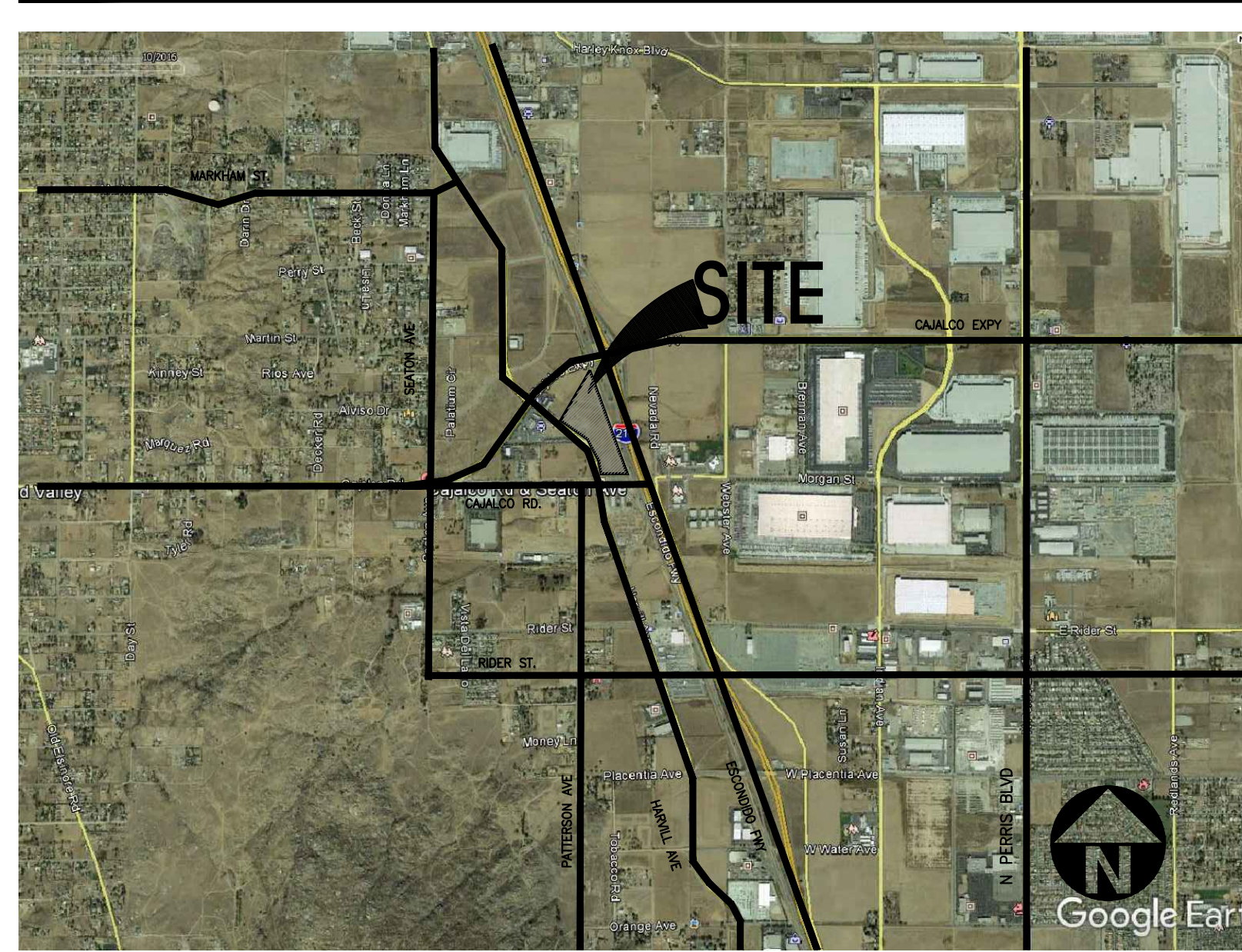
SITE PLAN GENERAL NOTES

- THE SOILS REPORT PREPARED BY SOUTHERN CALIFORNIA GEOTECHNICAL, INC. ON JULY 26, 2022 AND PROJECT NO.: 21G191-1, SHOULD BE A PART OF THESE CONTRACT DOCUMENTS.
- IF SOILS ARE EXPANSIVE IN NATURE, USE STEEL REINFORCING FOR ALL SITE CONCRETE.
- ALL DIMENSIONS ARE TO THE FACE OF CONCRETE WALL, FACE OF CONCRETE CURB OR GRID LINE U.N.O.
- SEE "C" PLANS FOR ALL CONCRETE CURBS, GUTTERS AND SWALES. DETAILS ON SHEET AD.1 ARE MINIMUM STANDARDS.
- THE ENTIRE PROJECT SHALL BE PERMANENTLY MAINTAINED WITH AN AUTOMATIC IRRIGATION SYSTEM, PRIOR TO INSTALLATION & AT LEAST 60 DAYS BEFORE BLDG. COMPLETION.
- SEE "C" DRAWINGS FOR POINT OF CONNECTIONS TO OFF-SITE UTILITIES. CONTRACTOR SHALL VERIFY ACTUAL UTILITY CONTRACTOR SHALL VERIFY ACTUAL UTILITY LOCATIONS. SEE "C" DRAWINGS.
- CONTRACTOR TO REFER TO "C" DRAWINGS FOR ALL HORIZONTAL CONTROL DIMENSIONS. SITE PLANS ARE FOR GUIDANCE AND STARTING LAYOUT POINTS.
- SEE "C" DRAWINGS FOR FINISH GRADE ELEVATIONS.
- CONCRETE SIDEWALKS TO BE A MINIMUM OF 4" THICK W/ TOOLED JOINTS AT 6' O.C. EXPANSION/CONSTRUCTION JOINTS SHALL BE A MAXIMUM 12' EX. WAY W/ 1:20 MAX. SLOPE. EXPANSION JOINTS TO HAVE COMPRESSIVE EXPANSION FILLER MATERIAL OF 1/4". SEE "L" DRAWINGS FOR FINISH.
- FOR TRUCK TURNING TEMPLATE SEE TRUCK TURNING EXHIBIT.
- PAINT CURBS AND PROVIDE SIGNS TO INFORM OF FIRE LANES AS REQUIRED BY FIRE DEPARTMENT.
- CONSTRUCTION DOCUMENTS PERTAINING TO THE LANDSCAPE AND IRRIGATION OF THE ENTIRE PROJECT SITE SHALL BE SUBMITTED TO THE BUILDING DEPARTMENT AND APPROVED BY PUBLIC FACILITIES DEVELOPMENT PRIOR TO ISSUANCE OF BUILDING PERMITS.
- PRIOR TO FINAL COUNTY INSPECTION, THE LANDSCAPE ARCHITECT SHALL SUBMIT A CERTIFICATE OF COMPLETION TO PUBLIC FACILITIES DEVELOPMENT.
- ALL LANDSCAPE AND IRRIGATION DESIGNS SHALL MEET CURRENT COUNTY STANDARDS AS LISTED IN GUIDELINES OR AS OBTAINED FROM PUBLIC FACILITIES DEVELOPMENT.
- LANDSCAPED AREAS SHALL BE DELINEATED WITH A MINIMUM SIX INCHES (6") HIGH CURB
- APPROVED CONCEPTUAL LANDSCAPE PLAN PRIOR TO GRADING PERMIT

SITE LEGEND

- | | |
|---|--|
| 1 HEAVY BROOM FINISH PORTLAND CONC. CEMENT PAVEMENT | 16 STANDARD PARKING STALL 9'-0" X 18' |
| 2 ASPHALT CONCRETE (AC) PAVING | 17 ACCESSIBLE PARKING STALL, 9' X 18' + 5' W ACCESSIBLE AISLE |
| 3 CONCRETE WALKWAY | 18 VAN ACCESSIBLE 12' X 18' + 5' W ACCESSIBLE AISLE |
| 4 DRIVEWAY APRONS TO BE CONSTRUCTED PER "L" DRAWINGS. | 19 CLEAN AIR VANPOOL/EV |
| 5 5'-6"x5'-6"x4" MIN. THICK CONCRETE EXTERIOR LANDING PAD TYP. AT ALL EXTERIOR MAN DOORS TO LANDSCAPED AREAS. FINISH TO BE MEDIUM BROOM FINISH. SLOPE TO BE 1/4" : 12" MAX. PROVIDE WALK TO PUBLIC WAY OR DRIVE WAY W/ 1:20 MAX. AS REQ. BY COUNTY INSPECTOR. | 20 EV PARKING ONLY PROVIDE CONDUIT FOR FUTURE EV CHARGING DESIGNATION (6 STALLS DAY 1) |
| 6 BREAK AREA PATIO. SEE LANDSCAPE PLANS | 21 PROVIDE CONDUIT FOR FUTURE EV CHARGING DESIGNATION (6 STALLS DAY 1) |
| 7 PROVIDE METAL MANUAL OPERATED GATES W/ KNOX-PAD LOCK PER FIRE DEPARTMENT STANDARDS PER DRIVEWAY. | 22 LIGHT STANDARD |
| 8 EXISTING SCREEN WALL TO REMAIN | 23 PRIVATE FIRE HYDRANT - APPROXIMATE LOCATION |
| 9 NOT USED | |
| 10 PRE-CAST CONC. WHEEL STOP | |
| 11 CONC. FILLED GUARD POST "6 DIA. U.N.O. 42" H. | |
| 12 EXTERIOR CONC. STAIR. | |
| 13 LANDSCAPE. SEE "L" DWGS. LANDSCAPE AREAS INDICATED BY SHADED PATTERN. | |
| 14 ACCESSIBLE ENTRY SIGN. | |
| 15 ACCESSIBLE PARKING STALL SIGN | |
| 16 BICYCLE RACKS | |
| 17 APPROXIMATE LOCATION OF THE TRANSFORMER | |
| 18 MAX 2:1 SLOPE. SEE CIVIL PLANS. | |
| 19 CONCRETE SIDEWALK. SEE CIVIL PLAN. | |
| 20 10'X55' TRAILER STALLS. | |
| 21 ROLLING GATE. | |
| 22 8' HIGH TUBULAR STEEL FENCE. RE: A/44.1 | |
| 23 CONCRETE RETAINING WALL. SEE CIVIL FOR DESIGN. PAINT TO MATCH PRIMARY BUILDING COLOR. | |

VICINITY MAP



HPA, inc.
18831 Bardeen Avenue, Ste. #100
Irvine, CA 92612
Tel: 949-863-1770
Fax: 949-863-0851
email: hpa@hparchs.com

Owner:
BCI IV HARVILL INDUSTRIAL CENTER LP

280 Newport Center Dr. Suite 240
Newport Beach, CA 92660
tel: 949-330-7564

Project:
BCI IV HARVILL INDUSTRIAL CENTER

Harvill Ave & Placentia
Riverside, CA 92570

Consultants:

- CIVIL: KIERWRIGHT
- STRUCTURAL: DFA
- MECHANICAL: DFA
- PLUMBING: DFA
- ELECTRICAL: DFA
- LANDSCAPE: HUNTER LANDSCAPE
- FIRE PROTECTION: HUNTER LANDSCAPE
- SOILS ENGINEER: HUNTER LANDSCAPE

Title: **OVERALL SITE PLAN**

PPT 220001

Project Number: 21137

Drawn by: AW/EM

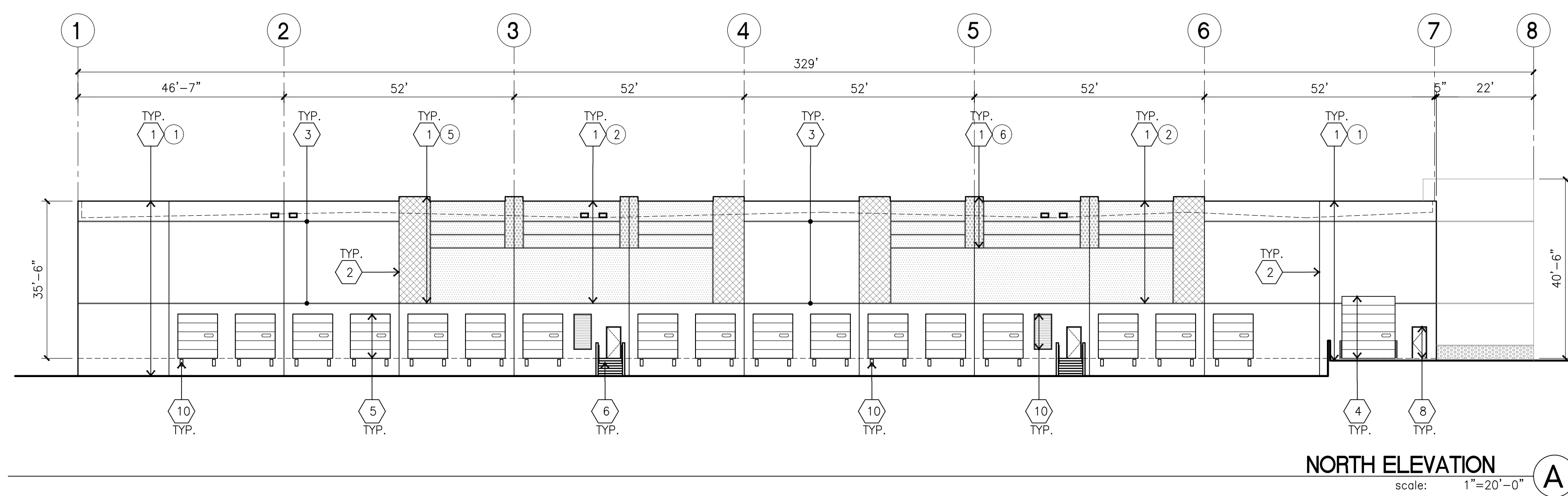
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Revision:

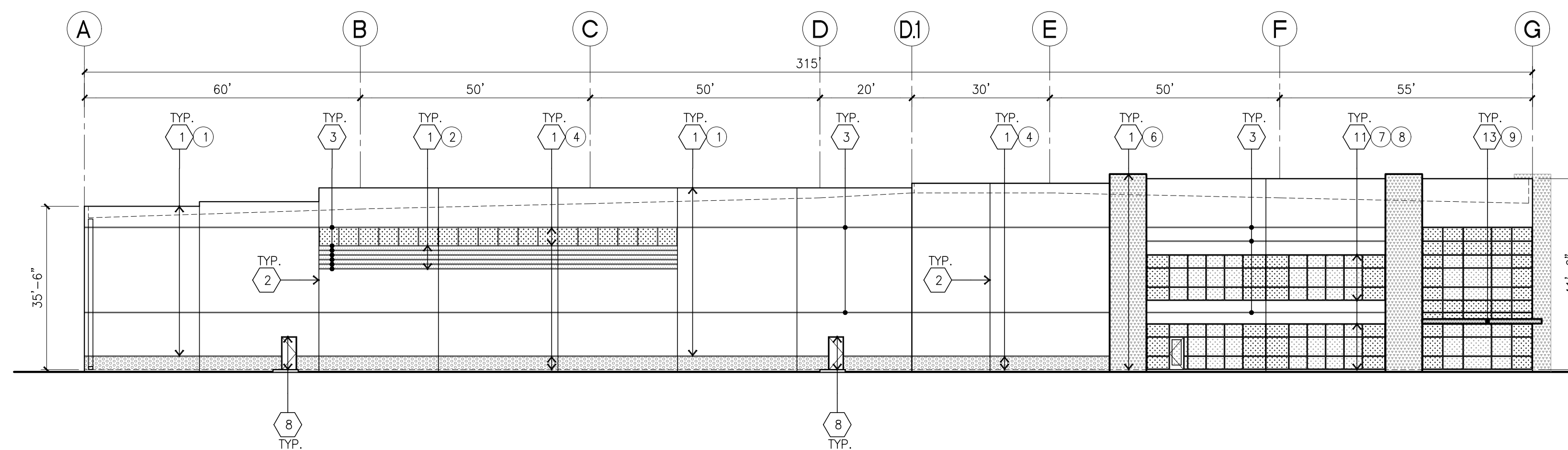
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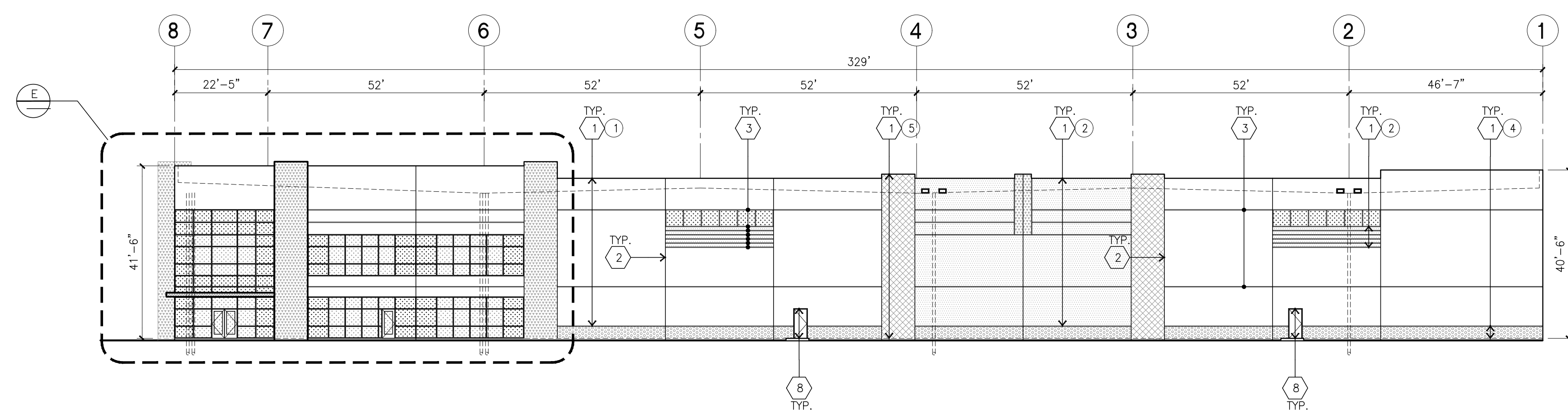
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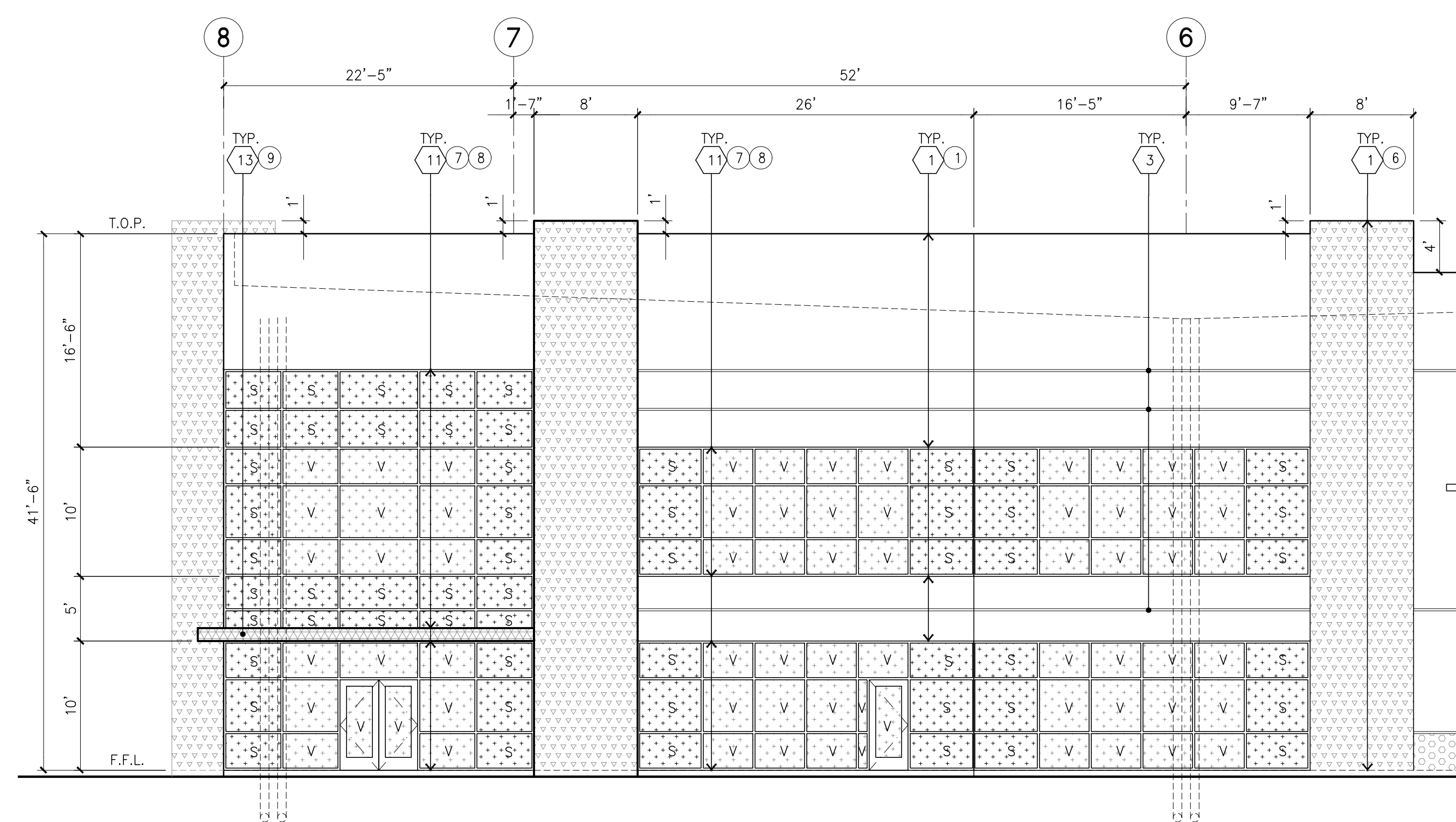
NORTH ELEVATION
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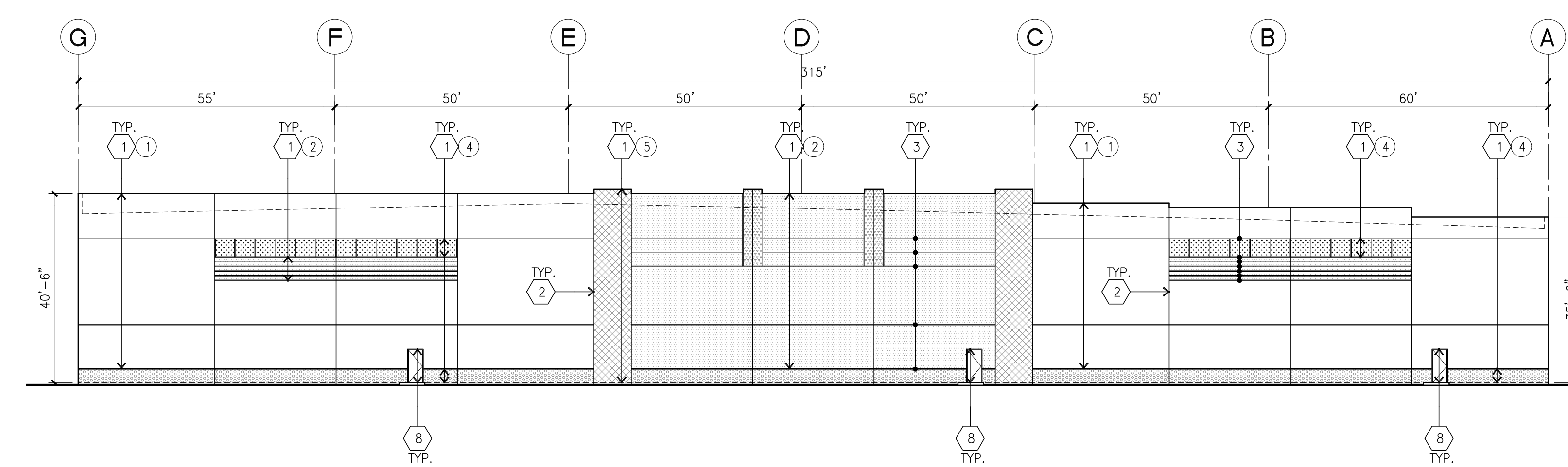
WEST ELEVATION
scale: 1"=20'-0"



SOUTH ELEVATION
scale: 1"=20'-0"



ENLARGED SOUTH ELEVATION
scale: 1/8"=1'-0"



EAST ELEVATION
scale: 1"=20'-0"

KEYNOTES - ELEVATIONS

- 1 CONCRETE TILT-UP PANEL (PAINTED). FINISH GRADE VARIES. SEE "C" DRAWINGS. WATERPROOF ALL WALLS WHERE GRADE IS HIGHER AND EXPOSED TO THE WEATHER ONE SIDE. WATERPROOFING TO BE PROTECTED WITH PROTECTION BOARD AND A MIN. OF 6" OF GRAVEL. PROVIDE TRENCH DRAIN AT BOTTOM AND DAYLIGHT TO CURB OR TAKE TO STORM DRAIN NOT REQUIRED AT DOCK HIGH CONDITION OR AT RAMP WALLS.
- 2 PANEL JOINT.
- 3 PANEL REVEAL. ALL REVEALS TO HAVE A MAX. OF 3/8" CHAMFER. REVEAL COLOR TO MATCH ADJACENT BUILDING FIELD COLOR. U.N.O.
- 4 OVERHEAD DOOR @ DRIVE THRU. SEE DOOR SCHEDULE. PROVIDE COMPLETE WEATHER-STRIPPING PROTECTION ALL AROUND. DESIGN TO RESIST 90 MPH WIND EXPOSURE "C".
- 5 OVERHEAD DOOR @ DOCK HIGH. SEE DOOR SCHEDULE. PROVIDE COMPLETE WEATHER-STRIPPING PROTECTION ALL AROUND. DESIGN TO RESIST 110 MPH WIND EXPOSURE "C".
- 6 CONCRETE STAIR, LANDING AND GUARDRAIL W/ METAL PIPE HANDRAIL. PROVIDE NON-SKID NOSING TO MEET ADA REQUIREMENTS. PROVIDE CONTRASTING COLORED 3" WIDE WARNING STRIPE INTEGRAL TO CONCRETE AT TOP LANDING AND BOTTOM TREAD PER ADA REQUIREMENTS.
- 7 METAL LOUVER. DESIGN TO RESIST 90 MPH WIND EXPOSURE "C". PAINT TO MATCH BUILDING COLOR.
- 8 HOLLOW METAL DOORS. SEE DOOR SCHEDULE. PROVIDE COMPLETE WEATHER STRIPPING ALL AROUND DOOR. PROVIDE FOR RAIN DIVERTER ABOVE DOOR. DESIGN TO RESIST 90 MPH WIND EXPOSURE "C".
- 9 EXTERIOR DOWNSPOUT AND OVERFLOW SCUPPER.
- 10 DOCK BUMPER.
- 11 ALUMINUM STOREFRONT FRAMING WITH TEMPERED GLAZING AT ALL DOORS. SIDELITES ADJACENT TO DOORS AND GLAZING WITH BOTTOMS LESS THAN 18" ABOVE FINISH FLOOR ELEVATION. DESIGN TO RESIST 90 MPH WIND EXPOSURE "C".
- 12 EXTERIOR LIGHTING FIXTURE.
- 13 METAL CANOPY.
- 14 INTERIOR ROOF DRAIN AND INTERIOR OVERFLOW.

GENERAL NOTES - ELEVATIONS

- A. ALL PAINT COLOR CHANGES TO OCCUR AT INSIDE CORNERS UNLESS NOTED OTHERWISE.
- B. ALL PAINT FINISHES ARE TO BE FLAT UNLESS NOTED OTHERWISE.
- C. T.O.P. EL. = TOP OF PARAPET ELEVATION.
- D. F.F. = FINISH FLOOR ELEVATION.
- E. STOREFRONT CONSTRUCTION: GLASS, METAL ATTACHMENTS AND LINTELS SHALL BE DESIGNED TO RESIST 90 MPH EXPOSURE "C" WINDS. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS PRIOR TO INSTALLATION.
- F. CONTRACTOR SHALL FULLY PAINT ONE CONCRETE PANEL W/ SELECTED COLORS. ARCHITECT AND OWNER SHALL APPROVE PRIOR TO PAINTING REMAINDER OF BUILDING.
- G. BACK SIDE OF PARAPETS TO HAVE SMOOTH FINISH AND BE PAINTED WITH ELASTOMERIC PAINT.
- H. FOR SPANDREL GLAZING. ALLOW SPACE BEHIND SPANDREL TO BREATHE.
- J. USE ADHESIVE BACK WOOD STRIPS FOR ALL REVEAL FORMS.
- K. THE FIRST COAT OF PAINT TO BE ROLLED-ON AND THE SECOND COAT TO BE SPRAYED-ON.

COLOR SCHED. - ELEVATIONS

1	CONCRETE TILT-UP PANEL PAINT BRAND_PURE WHITE	SHERWIN WILLIAMS SW7005
2	CONCRETE TILT-UP PANEL PAINT BRAND_GRAY SCREEN	SHERWIN WILLIAMS SW7071
3	CONCRETE TILT-UP PANEL PAINT BRAND_ONLINE	SHERWIN WILLIAMS SW7072
4	CONCRETE TILT-UP PANEL PAINT BRAND_NETWORK GRAY	SHERWIN WILLIAMS SW7073
5	CONCRETE TILT-UP PANEL PAINT BRAND_WEB GRAY	SHERWIN WILLIAMS SW7075
6	CONCRETE TILT-UP PANEL PAINT BRAND_INDIGO BATIK	SHERWIN WILLIAMS SW 7602
7	MULLIONS	COLOR_CLEAR ANODIZED MULLIONS
8	GLAZING	COLOR_BLUE REFLECTIVE GLAZING
9	CANOPIES/METAL	SHERWIN WILLIAMS METALLICS SW7005 PAINT BRAND_PURE WHITE (ACRYLIC LATEX SYSTEMS)

GLAZING LEGEND

[Symbol]	SPANDREL GLASS	[Symbol]	TEMPERED VISION GLASS
[Symbol]	VISION GLASS		



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BCI IV HARVILL INDUSTRIAL CENTER LP

280 Newport Center Dr. Suite 240
Newport Beach, CA 92660

tel: 949-330-7564

Project:

BCI IV HARVILL INDUSTRIAL CENTER

Harvill Ave & Placentia
Riverside, CA 92570

Consultants:

CIVIL KIERWRIGHT
STRUCTURAL DFA
MECHANICAL
PLUMBING
ELECTRICAL
LANDSCAPE HUNTER LANDSCAPE
FIRE PROTECTION
SOILS ENGINEER

Title: ELEVATIONS

PPT 220001

Project Number: 21137

Drawn by: AW/EM

Date: 4/25/22

Revision:

Sheet:

DAB-A3.1



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Owner:

BCI IV HARVILL
INDUSTRIAL
CENTER LP

280 Newport Center Dr. Suite 240
Newport Beach, CA 92660
tel: 949-330-7564

Project:

BCI IV HARVILL
INDUSTRIAL
CENTER

Harvill Ave & Placentia
Riverside, CA 92570

Consultants:

CIVIL	KIERWRIGHT
STRUCTURAL	DFA
MECHANICAL	
PLUMBING	
ELECTRICAL	
LANDSCAPE	HUNTER LANDSCAPE
FIRE PROTECTION	
SOILS ENGINEER	

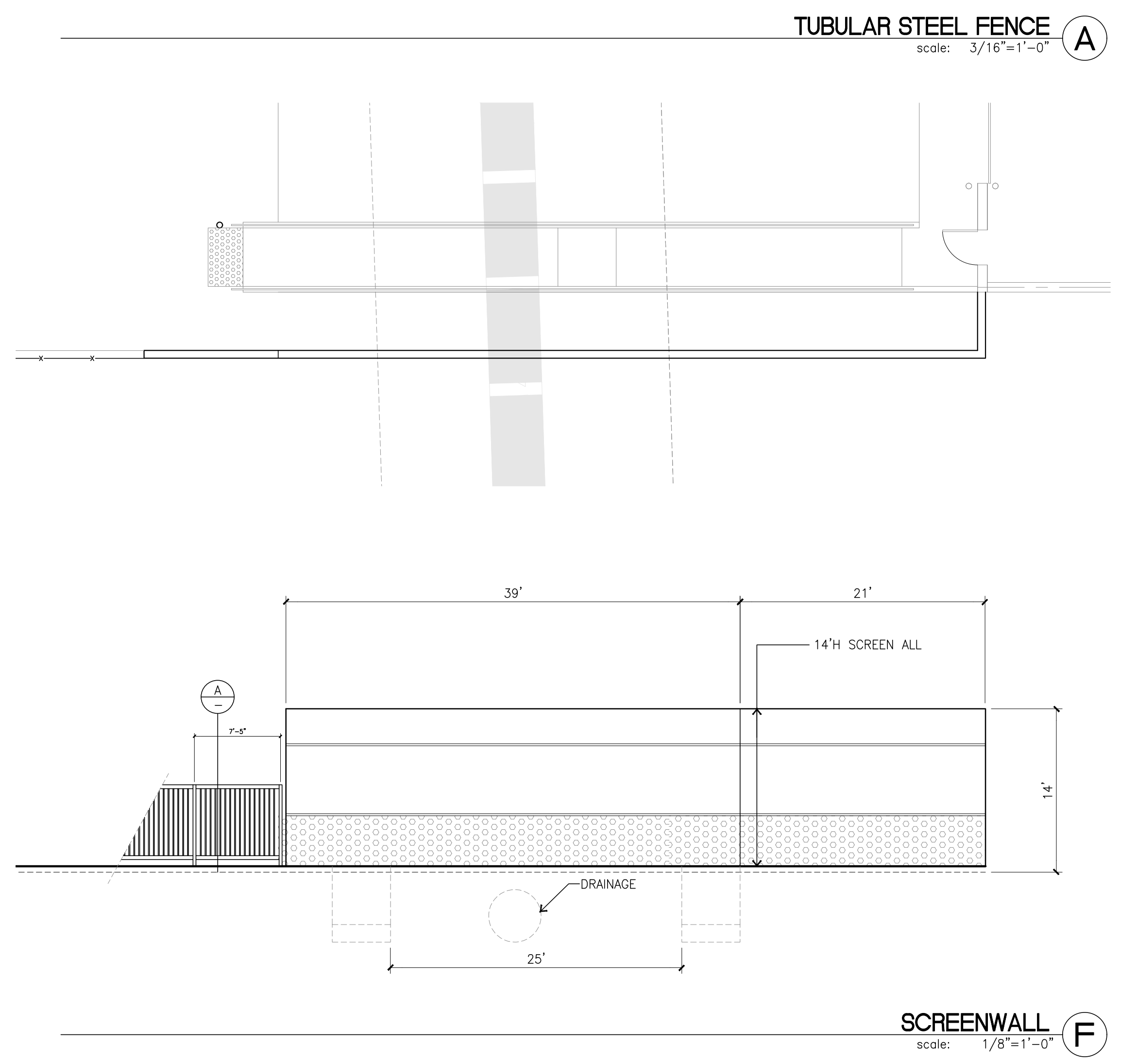
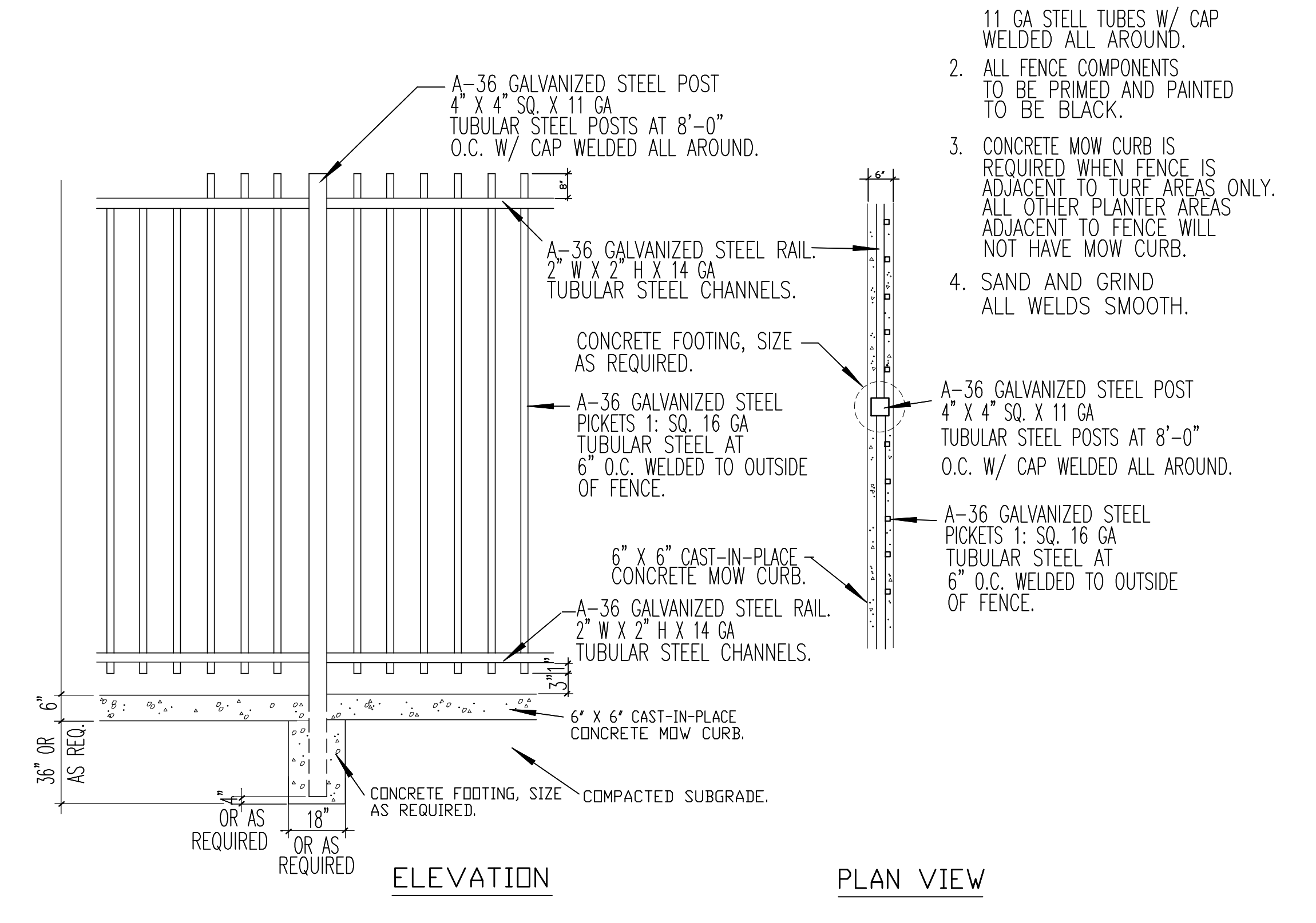
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PPT 220001

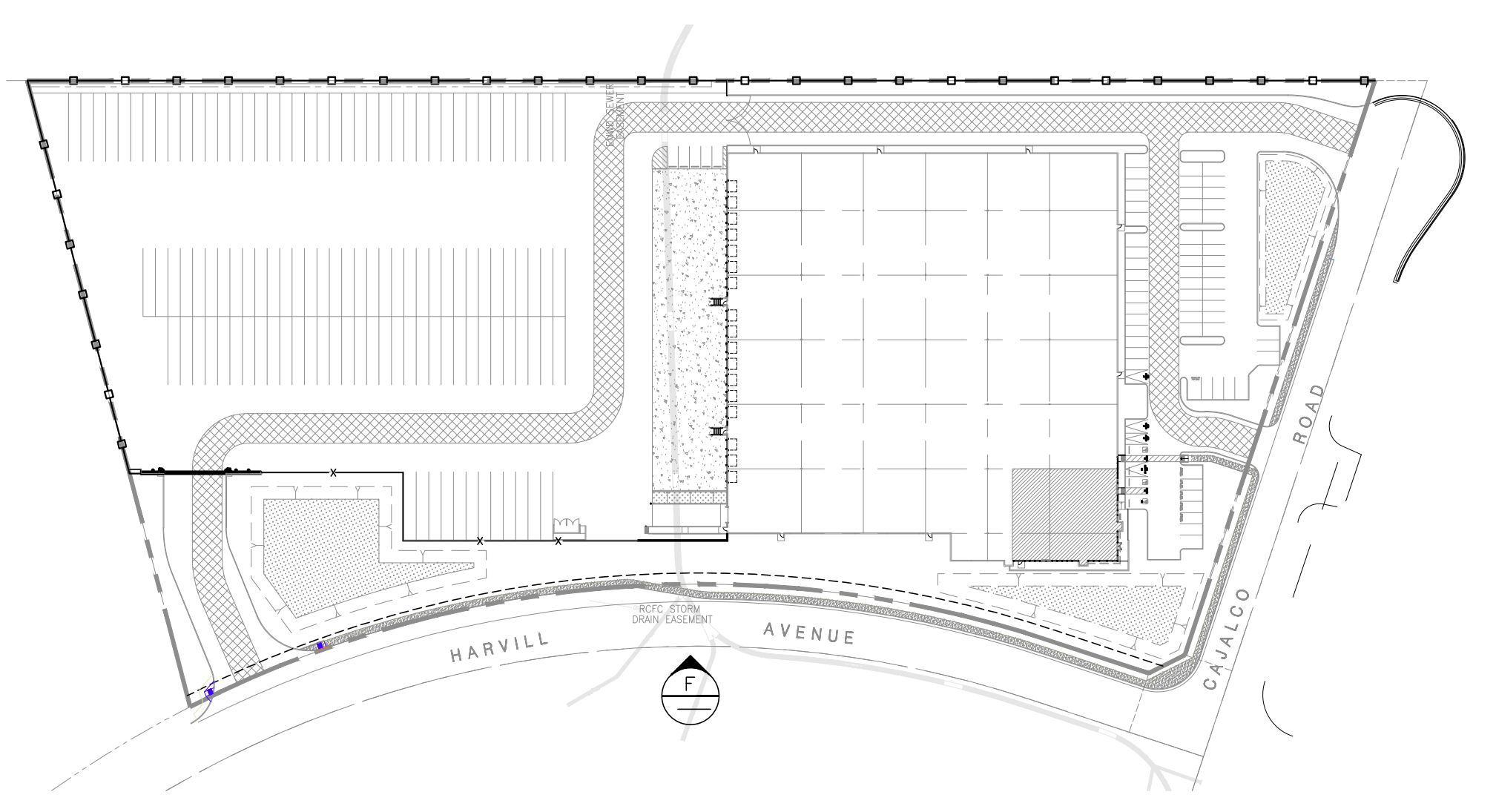
Project Number: 21137
Drawn by: AW/EM
Date: 4/25/22
Revision:

Sheet:

DAB-A4.1



KEY MAP



ENVIRONMENT | PLANNING | DEVELOPMENT SOLUTIONS, INC.

Date: April 26, 2022
Prepared by: Abby Pal
To: Michael Brown, Project Manager
Site: Harvill Avenue and Cajalco Road Warehouse Project
Subject: Rooftop Solar Panel Installation Glare Analysis

This technical memorandum evaluates the potential solar glare resulting from the installation of solar panels on the roof of the proposed 99,770 square feet warehouse at the northeast corner of the intersection of Harvill Avenue and Cajalco Road in the City of Perris. The project is within the March Air Reserve Base (March ARB) airport influence area. The solar glare analysis assesses the compatibility of the proposed solar panels as per the requirements of March ARB. The analysis and findings of this memo are intended for review and acceptance by Riverside County, Riverside County Airport Land Use Commission (ALUC) and the March ARB.

Project Description

As mentioned above, the proposed solar panel installation would be located at the northeast corner of the intersection of Harvill Avenue and Cajalco Road in the City of Perris. The location of the project with respect to March ARB is shown in *Attachment A*. The site plan of the proposed warehouse project is shown in *Attachment B*. The solar glare analysis approximately assumes 99,800 square feet rooftop coverage which would cover 100% of the proposed warehouse building roof for a conservative analysis. The rooftop height for the proposed warehouse development is 40.5 feet. The solar panel installation was assumed to be smooth glass with anti-reflective coating, fixed mounted at 3 feet above the ground with a tilt of 33.0° and orientated at 160.0°.

Solar Glare Analysis Tools and Methodology

The potential impact of glint and glare from photovoltaic modules, concentrating solar collectors, receivers, and other components has received increased attention as a potential hazard or distraction for pilots, air-traffic control and other airport personnel. Hazards from reflected solar radiation include the potential for permanent eye injury (e.g., retinal burn from concentrated sunlight) and temporary disability or distractions (e.g., glint, glare, after-images). The Federal Aviation Administration (FAA) requires the sponsor of proposed construction or alteration to confirm that the potential for glint and glare has been analyzed and determined to have no potential for ocular impact to the airport's Air Traffic Control Tower (ATCT) cab ¹.

Sandia National Laboratories (National Technology and Engineering Solutions of Sandia, LLC.) developed early Solar Glare Hazard Analysis Tools (SGHAT) which included programs for modeling and analyzing potential hazards from solar glare. These tools and programs had been adopted as a standard for FAA and other airport/user reviews. Due to new cybersecurity restrictions at Sandia, SGHAT is now available for internal Sandia use only. All external use of SGHAT is restricted, however the glare tool source code and algorithms were made available for licensing. The FAA policy to demonstrate compliance with 14 CFR 77.5 (c)² updated on May 11, 2021 withdrew their recommendation of the Solar Glare Hazard Analysis Tool (SGHAT) to analyze ocular impact as the tool is no longer available to all users at no cost. However, EPD

¹ Federal Register. Referenced at <https://www.federalregister.gov/documents/2021/05/11/2021-09862/federal-aviation-administration-policy-review-of-solar-energy-system-projects-on-federally-obligated>

² Code of Federal Regulations. Referenced at [https://www.ecfr.gov/current/title-14/chapter-I/subchapter-E/part-77/subpart-B/section-77.5#p-77.5\(c\)](https://www.ecfr.gov/current/title-14/chapter-I/subchapter-E/part-77/subpart-B/section-77.5#p-77.5(c))

obtained the use of ForgeSolar Glare Analysis tool which utilizes the SGHAT glare tool source code and algorithms under subscription made available by Sims Industries (d/b/a ForgeSolar) offering comparable tools for FAA-certifiable glare analysis.

This solar glare analysis memo incorporates the following:

- 1) Potential for glint or glare in the existing or planned Airport Traffic Control Tower (ATCT) cab.
- 2) Potential for glare or “low potential for after-image” along the final approach path for any existing landing threshold or future landing thresholds. The final approach path is defined as 2 miles from 50 feet above the landing threshold using a standard 3° glidepath.
- 3) Ocular impact analyzed over the entire calendar year in one-minute intervals from when the sun rises above the horizon until the sun sets below the horizon.

In addition to the above, this memo also analyzes a series of air traffic patterns for the two existing runways at March ARB established by the Riverside County ALUC staff in conjunction with March ARB staff. This is to ensure that the land uses around the base are compatible with its air operations and that the proposed solar panel installations will not create a hazard to air navigation as a result of reflected sunlight and the associated potential glare. The following March ARB standard traffic patterns have been included for analysis:

- Runway 12/30 General Aviation Traffic Pattern
- Runway 14/32 General Aviation Traffic Pattern
- Runway 14/32 C-17/KC-135 Traffic Pattern
- Runway 14/32 Overhead Traffic Pattern

Findings

The light reflected from the surface of solar panels can result in glint (a momentary flash of bright light) and glare (a continuous source of bright light). These two effects can cause a brief loss of vision which can hamper the safe maneuvering of the aircraft while in flight. Ocular impact from solar glare can result in green glare or yellow glare. Green glare can be defined as glare with low potential to cause after image or flash blindness for a few seconds which would not hamper safe aircraft maneuvering. Yellow glare can be defined as glare with potential to cause temporary after-image lasting more than a few seconds that might hamper safe aircraft maneuvering. Yellow glare is not acceptable as per glare hazard model criteria and would require mitigation to reduce ocular impact to green glare or better.

As per the solar glare analysis conducted, the proposed rooftop solar panel installation would result in green glare as shown in Table 1 below. No yellow glare is predicted. Therefore, the proposed rooftop solar panel installation would pass the glare hazard model criteria, with zero minutes per year outside the ‘green zone’ of acceptable reflected solar energy. The glare analysis results for all flight paths and the ocular impact on ATCT is presented in *Attachment C*.

Table 1: Glare Minutes per Year

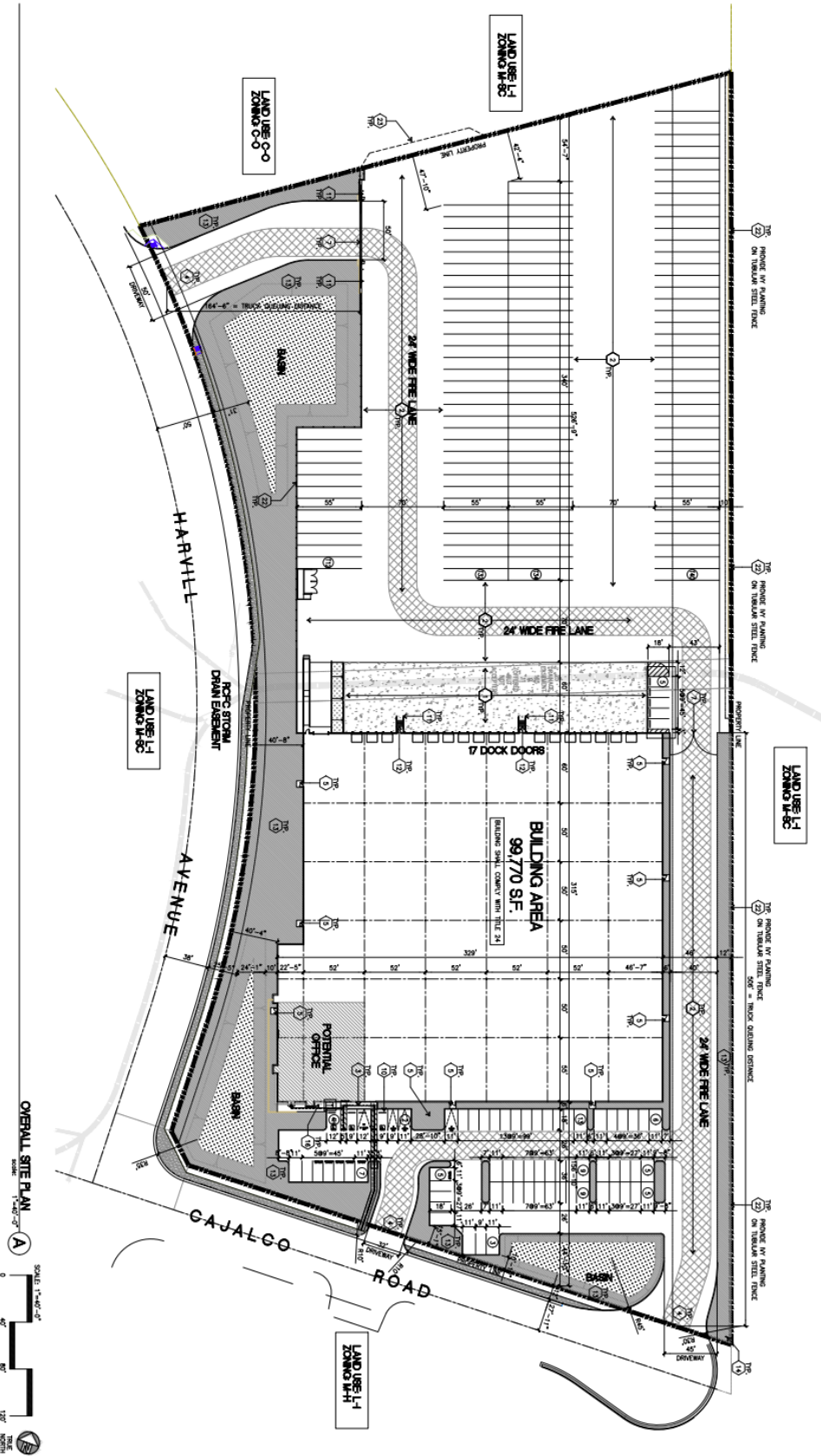
Analysis Component	Green Glare (min)	Yellow Glare (min)	Pass?
1. Runway 12 Final	0	0	Yes
2. Runway 14 Final	0	0	Yes
3. Runway 30 Final	0	0	Yes
4. Runway 32 Final	0	0	Yes
5. ATCT	0	0	Yes
6. Runway 12-30 General Aviation Traffic Pattern (two-way)	0	0	Yes
7. Runway 14-32 General Aviation Traffic Pattern (two-way)	0	0	Yes
8. Runway 14-32 C-17 KC-135 Rectangular Analysis (two-way)	621	0	Yes
9. Runway 14 Overhead Analysis (one-way)	0	0	Yes
10. Runway 32 Overhead Analysis (one-way)	5922	0	Yes

If you have any questions about this information, please contact me at (412) 636-2713 or abby@epdsolutions.com.

Attachment A: Solar Panel Location



Attachment B: Project Site Plan



OVERALL SITE PLAN
 SCALE: 1"=40'-0"
 NORTH

Attachment C: Harvill and Cajalco Solar Glare Analysis Results

ForgeSolar Cookie Policy

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ForgeSolar

March ARB Solar Glare Analysis

21-118 Harvill and Cajalco-temp-1

Created April 25, 2022

Updated April 25, 2022

Time-step 1 minute

Timezone offset UTC-8

Site ID 68115.11944

Project type Basic

Project status: active

Category 500 kW to 1 MW

(1,000 kW / 8 acre limit)



Misc. Analysis Settings

DNI: varies (1,000.0 W/m² peak)

Ocular transmission coefficient: **0.5**

Pupil diameter: **0.002 m**

Eye focal length: **0.017 m**

Sun subtended angle: **9.3 mrad**

Analysis Methodologies:

- Observation point: **Version 2**
- 2-Mile Flight Path: **Version 2**
- Route: **Version 2**

Summary of Results Glare with low potential for temporary after-image predicted

PV Name	Tilt	Orientation	"Green" Glare	"Yellow" Glare	Energy Produced
	deg	deg	min	min	kWh
Harvill and Cajalco Rooftop	33.0	160.0	6,543	0	-

Component Data

PV Array(s)

Total PV footprint area: 2.3 acres

Name: Harvill and Cajalco Rooftop
Description: Rooftop Solar Panel Array
Footprint area: 2.3 acres
Axis tracking: Fixed (no rotation)
Tilt: 33.0 deg
Orientation: 160.0 deg
Rated power: -
Panel material: Smooth glass with AR coating
Vary reflectivity with sun position? Yes
Correlate slope error with surface type? Yes
Slope error: 8.43 mrad

Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.838778	-117.251436	1504.01	43.50	1547.52
2	33.839072	-117.250438	1507.21	43.50	1550.71
3	33.838291	-117.250082	1505.93	43.50	1549.44
4	33.837972	-117.251079	1507.86	43.50	1551.36



2-Mile Flight Path Receptor(s)

Name: Rwy 12 Final

Description:

Threshold height : 50 ft

Direction: 135.0 deg

Glide slope: 3.0 deg

Pilot view restricted? Yes

Vertical view restriction: 30.0 deg

Azimuthal view restriction: 50.0 deg



Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.889854	-117.260195	1519.17	50.00	1569.18
2-mile point	33.910298	-117.284852	1519.17	603.46	2122.63

Name: Rwy 14

Description:

Threshold height : 50 ft

Direction: 149.0 deg

Glide slope: 3.0 deg

Pilot view restricted? Yes

Vertical view restriction: 30.0 deg

Azimuthal view restriction: 50.0 deg



Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.895961	-117.270345	1535.57	50.00	1585.58
2-mile point	33.920744	-117.288306	1535.57	603.46	2139.03

Name: Rwy 30 Final

Description:

Threshold height : 50 ft

Direction: 315.0 deg

Glide slope: 3.0 deg

Pilot view restricted? Yes

Vertical view restriction: 30.0 deg

Azimuthal view restriction: 50.0 deg



Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.884521	-117.253778	1507.17	50.00	1557.18
2-mile point	33.864063	-117.229139	1507.17	603.46	2110.63

Name: Rwy 32 Final

Description:

Threshold height : 50 ft

Direction: 329.0 deg

Glide slope: 3.0 deg

Pilot view restricted? Yes

Vertical view restriction: 30.0 deg

Azimuthal view restriction: 50.0 deg

Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.865170	-117.248411	1488.37	50.00	1538.38
2-mile point	33.840395	-117.230441	1488.37	603.46	2091.83



Route Receptor(s)

Name: Rwy 12-30 GA Rectangular Analysis

Route type: Two-way

View angle: 50.0 deg



Vertex	Latitude deg	Longitude deg	Ground elevation ft	Height above ground ft	Total elevation ft
1	33.884319	-117.253536	1500.07	50.00	1550.08
2	33.876069	-117.243611	1500.07	1300.06	2800.14
3	33.876081	-117.235119	1500.07	1300.06	2800.14
4	33.880814	-117.229467	1500.07	1300.06	2800.14
5	33.887897	-117.229483	1500.07	1300.06	2800.14
6	33.910333	-117.256469	1500.07	1300.06	2800.14
7	33.910322	-117.264967	1500.07	1300.06	2800.14
8	33.905592	-117.270622	1500.07	1300.06	2800.14
9	33.898508	-117.270608	1500.07	1300.06	2800.14
10	33.890258	-117.260681	1500.07	50.00	1550.08

Name: Rwy 14-32 C-17 - KC-135 Rectangular Analysis

Route type: Two-way

View angle: 50.0 deg



Vertex	Latitude deg	Longitude deg	Ground elevation ft	Height above ground ft	Total elevation ft
1	33.864994	-117.248281	1500.07	50.00	1550.08
2	33.836269	-117.227869	1500.07	1300.06	2800.14
3	33.821961	-117.228367	1500.07	1300.06	2800.14
4	33.813147	-117.244350	1500.07	1300.06	2800.14
5	33.819225	-117.262269	1500.07	1300.06	2800.14
6	33.908131	-117.325528	1500.07	1300.06	2800.14
7	33.922394	-117.325047	1500.07	1300.06	2800.14
8	33.931244	-117.309014	1500.07	1300.06	2800.14
9	33.925156	-117.291061	1500.07	1300.06	2800.14
10	33.896431	-117.270636	1500.07	50.00	1550.08

Name: Rwy 14-32 GA Rectangular Analysis

Route type: Two-way

View angle: 50.0 deg

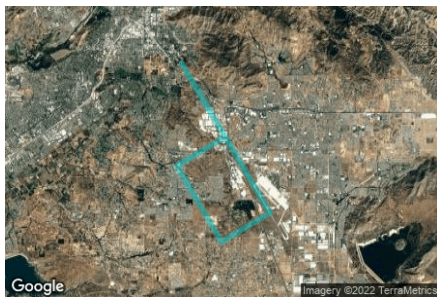


Vertex	Latitude deg	Longitude deg	Ground elevation ft	Height above ground ft	Total elevation ft
1	33.864994	-117.248281	1500.07	50.00	1550.08
2	33.854942	-117.241136	1500.07	1300.06	2800.14
3	33.848078	-117.243236	1500.07	1300.06	2800.14
4	33.844669	-117.250119	1500.07	1300.06	2800.14
5	33.846422	-117.258344	1500.07	1300.06	2800.14
6	33.897972	-117.295011	1500.07	1300.06	2800.14
7	33.904833	-117.292903	1500.07	1300.06	2800.14
8	33.908242	-117.286017	1500.07	1300.06	2800.14
9	33.906486	-117.277783	1500.07	1300.06	2800.14
10	33.896431	-117.270636	1500.07	50.00	1550.08

Name: Rwy 14 Overhead Analysis

Route type: One-way

View angle: 50.0 deg



Vertex	Latitude deg	Longitude deg	Ground elevation ft	Height above ground ft	Total elevation ft
1	33.968036	-117.322128	1500.07	2000.10	3500.17
2	33.880706	-117.259453	1500.07	2000.10	3500.17
3	33.863564	-117.293808	1500.07	2000.10	3500.17
4	33.908131	-117.325528	1500.07	2000.10	3500.17
5	33.925156	-117.291061	1500.07	2000.10	3500.17
6	33.896431	-117.270636	1500.07	50.00	1550.08

Name: Rwy 32 Overhead
Route type: One-way
View angle: 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.793375	-117.196878	1500.07	2000.10	3500.17
2	33.880706	-117.259453	1500.07	2000.10	3500.17
3	33.863564	-117.293808	1500.07	2000.10	3500.17
4	33.819225	-117.262269	1500.07	2000.10	3500.17
5	33.836269	-117.227869	1500.07	2000.10	3500.17
6	33.864994	-117.248281	1500.07	50.00	1550.08

Discrete Observation Receptors

Number	Latitude	Longitude	Ground elevation	Height above ground	Total Elevation
	deg	deg	ft	ft	ft
1-ATCT	33.891569	-117.251175	1511.07	118.01	1629.08

1-ATCT map image



Summary of PV Glare Analysis

PV configuration and total predicted glare

PV Name	Tilt	Orientation	"Green" Glare	"Yellow" Glare	Energy Produced	Data File
	deg	deg	min	min	kWh	
Harvill and Cajalco Rooftop	33.0	160.0	6,543	0	-	-

Distinct glare per month

Excludes overlapping glare from PV array for multiple receptors at matching time(s)

PV	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec
harvill-and (green)	0	0	528	1022	967	957	981	952	951	0	0	0
harvill-and (yellow)	0	0	0	0	0	0	0	0	0	0	0	0

PV & Receptor Analysis Results

Results for each PV array and receptor

Harvill and Cajalco Rooftop low potential for temporary after-image

Component	Green glare (min)	Yellow glare (min)
FP: Rwy 12 Final	0	0
FP: Rwy 14	0	0
FP: Rwy 30 Final	0	0
FP: Rwy 32 Final	0	0
OP: 1-ATCT	0	0
Route: Rwy 12-30 GA Rectangular Analysis	0	0
Route: Rwy 14-32 C-17 - KC-135 Rectangular Analysis	621	0
Route: Rwy 14-32 GA Rectangular Analysis	0	0
Route: Rwy 14 Overhead Analysis	0	0
Route: Rwy 32 Overhead	5922	0

Harvill and Cajalco Rooftop - Receptor (Rwy 12 Final)

No glare found

Harvill and Cajalco Rooftop - Receptor (Rwy 14)

No glare found

Harvill and Cajalco Rooftop - Receptor (Rwy 30 Final)

No glare found

Harvill and Cajalco Rooftop - Receptor (Rwy 32 Final)

No glare found

Harvill and Cajalco Rooftop - OP Receptor (1-ATCT)

No glare found

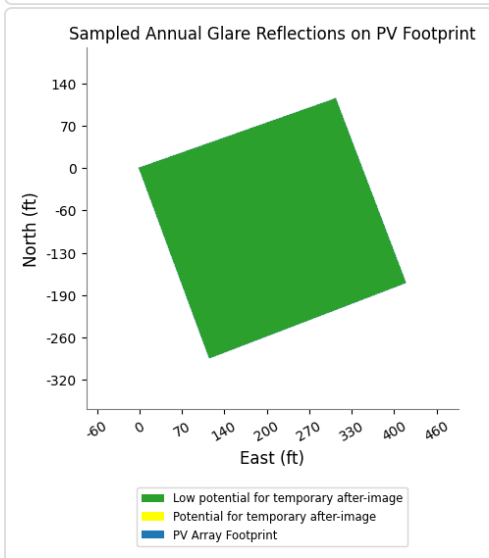
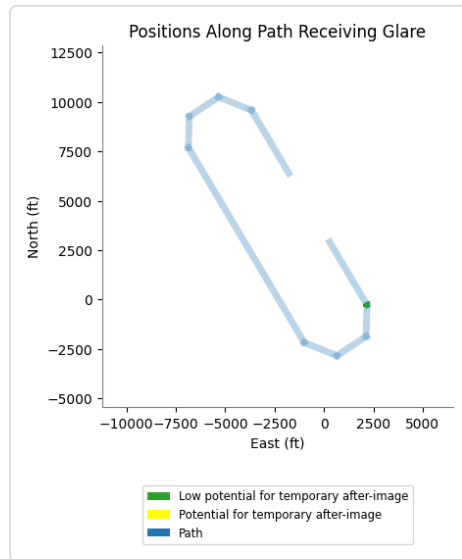
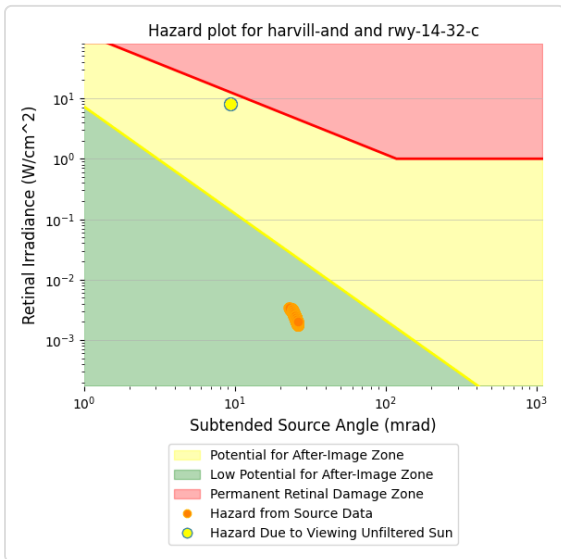
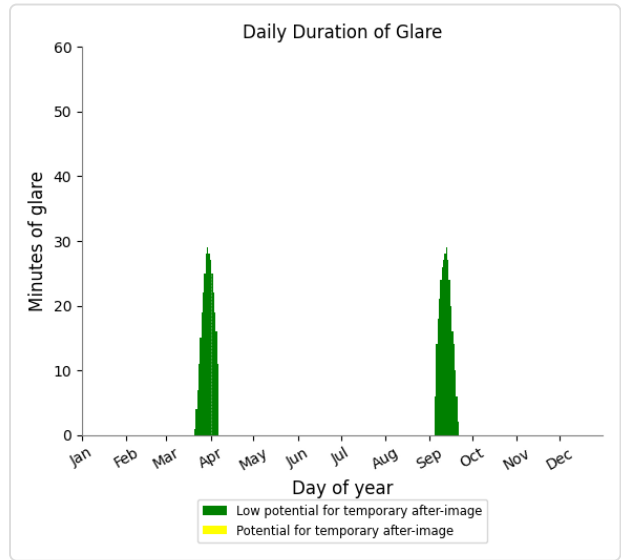
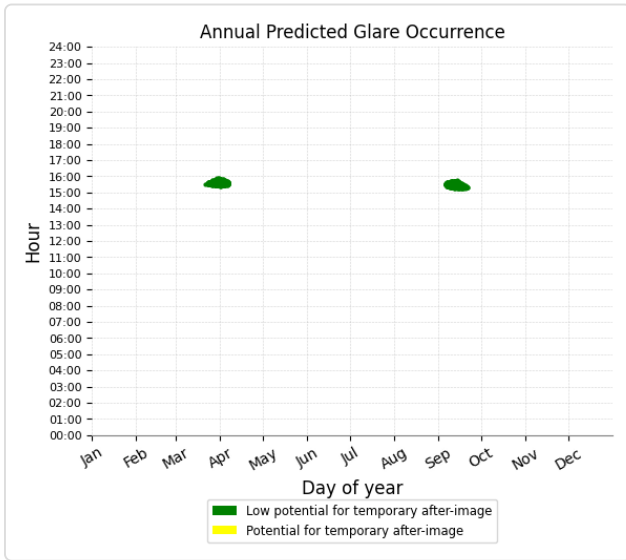
Harvill and Cajalco Rooftop - Route Receptor (Rwy 12-30 GA Rectangular Analysis)

No glare found

Harvill and Cajalco Rooftop - Route Receptor (Rwy 14-32 C-17 - KC-135 Rectangular Analysis)

PV array is expected to produce the following glare for receptors at this location:

- 621 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



Harvill and Cajalco Rooftop - Route Receptor (Rwy 14-32 GA Rectangular Analysis)

No glare found

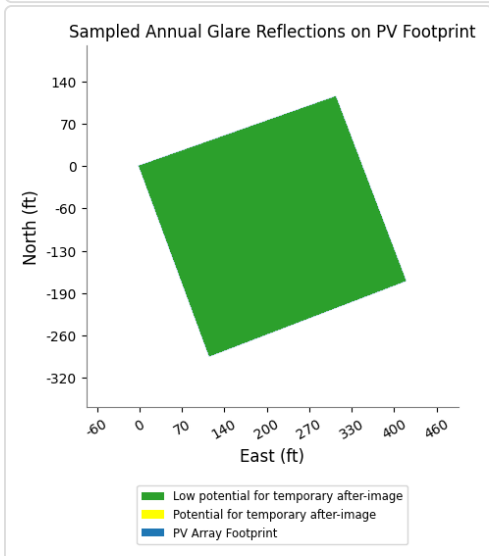
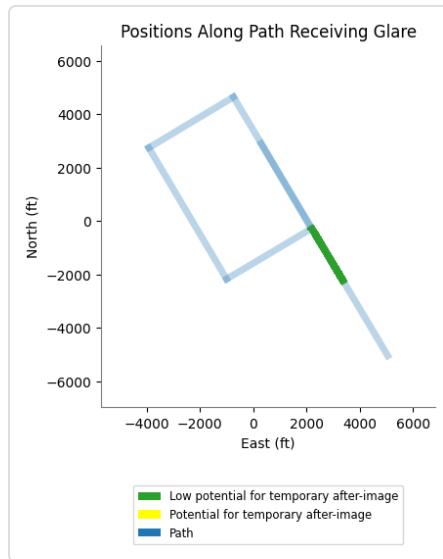
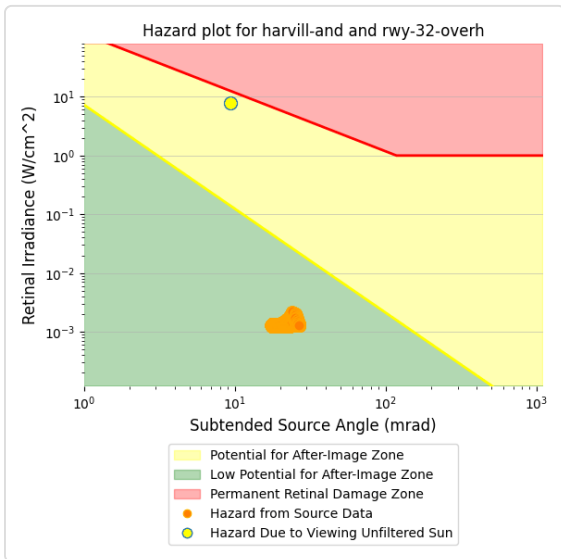
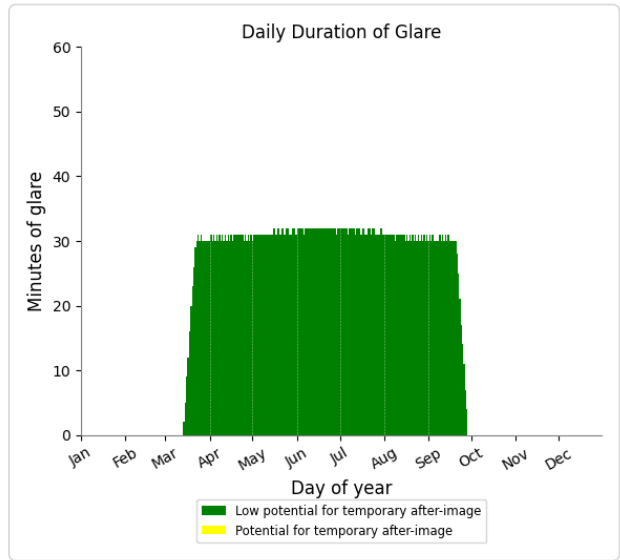
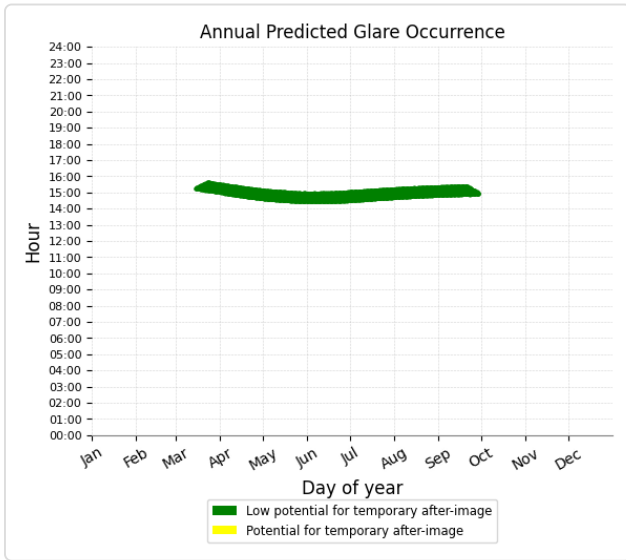
Harvill and Cajalco Rooftop - Route Receptor (Rwy 14 Overhead Analysis)

No glare found

Harvill and Cajalco Rooftop - Route Receptor (Rwy 32 Overhead)

PV array is expected to produce the following glare for receptors at this location:

- 5,922 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



Assumptions

- Times associated with glare are denoted in Standard time. For Daylight Savings, add one hour.
- Glare analyses do not account for physical obstructions between reflectors and receptors. This includes buildings, tree cover and geographic obstructions
- Detailed system geometry is not rigorously simulated.
- The glare hazard determination relies on several approximations including observer eye characteristics, angle of view, and typical blink response time. Actual values and results may vary.
- The system output calculation is a DNI-based approximation that assumes clear, sunny skies year-round. It should not be used in place of more rigorous modeling methods.
- Several V1 calculations utilize the PV array centroid, rather than the actual glare spot location, due to algorithm limitations. This may affect results for large PV footprints. Additional analyses of array sub-sections can provide additional information on expected glare.
- The subtended source angle (glare spot size) is constrained by the PV array footprint size. Partitioning large arrays into smaller sections will reduce the maximum potential subtended angle, potentially impacting results if actual glare spots are larger than the sub-array size. Additional analyses of the combined area of adjacent sub-arrays can provide more information on potential glare hazards. (See previous point on related limitations.)
- Hazard zone boundaries shown in the Glare Hazard plot are an approximation and visual aid. Actual ocular impact outcomes encompass a continuous, not discrete, spectrum.
- Glare locations displayed on receptor plots are approximate. Actual glare-spot locations may differ.
- Glare vector plots are simplified representations of analysis data. Actual glare emanations and results may differ.
- Refer to the **Help page** for detailed assumptions and limitations not listed here.

NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. **Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org.** The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893.**

The County of Riverside Planning Department should be contacted on non-ALUC issues. For more information please contact County of Riverside Planner Deborah Bradford at (951) 922-6645.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to prull@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please contact Barbara Santos at (951) 955-5132.

PLACE OF HEARING: **Riverside County Administration Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside California**

DATE OF HEARING: **July 14, 2022**

TIME OF HEARING: **9:30 A.M.**

CASE DESCRIPTION:

ZAP1517MA22 – BCI IV Harvill Industrial Center (Representative: EPD Solutions) – County of Riverside Case No. PPT220001 (Plot Plan). A proposal to construct a 99,770 square foot manufacturing building on 9.13 acres located northerly of Cajalco Road, westerly of Interstate 215, easterly of Harvill Avenue, and southerly of Dree Circle. The applicant also proposes to construct a rooftop solar panel system on the proposed building (Airport Compatibility Zone C2 of the March Air Reserve Base/Inland Port Airport Influence Area).



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC STAFF ONLY

ALUC Case Number: ZAP1517MA22

Date Submitted: 4/26/22

AIA: March

Zone: C2

Public Hearing Staff Review

Applicant

Applicant Full Name: BCI IV Harvill Industrial Center

Applicant Address: 4675 MacArthur Court, Suite 625 Newport Beach, 92660

Phone: 949-892-4904 Email: pschafer@aresmgmt.com

Representative/ Property Owner Contact Information

Representative: EPD Solutions - Michael Brown

Email: michael@epdsolutions.com

Phone: 951-639-7689

Address: 2355 Main Street, Suite 100 Irvine CA 92614

Property Owner: Same as applicant - see above.

Email: _____

Phone: _____

Address: _____

Local Jurisdiction Agency

Agency Name: Riverside County

Phone: 951-955-6646

Staff Contact: Deborah Bradford

Email: dbradfor@rivco.org

Address: 4080 Lemon Street, 12th Floor Riverside CA 92501

Local Agency Case No.: PPT220001

Project Location

Street Address: NEC Harvill Ave and Cajalco Rd

Gross Parcel Size: 9.13

Assessor's Parcel No.: 317-130-034, -035

Solar

Is the project proposing solar Panels? Yes



No



If yes, please provide solar glare study. (only if in Zone C or higher)

Data

Site Elevation:(above mean sea level) +/- 1,508

Height of Building or structures: 40'-6"

What type of drainage basins are being proposed and the square footage: Bio-Retention Basin(s) | 28,250 SF

Notice

A. NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. REVIEW TIME: Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of a complete application submittal to the next available commission hearing meeting.

C. SUBMISSION PACKAGE:

Please submit all application items DIGITALLY via USB or CD:

- Completed ALUC Application Form
- Plans Package: site plans, floor plans, building elevations, grading plans, subdivision maps
- Exhibits of change of zone, general plan amendment, specific plan amendment
- Project description of existing and proposed use

Additionally, please provide:

- ALUC fee payment (Checks made out to Riverside County ALUC)
- Gummed address labels of all surrounding property owners within a 300-foot radius of project site. (Only required if the project is scheduled for a public hearing).

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM:	3.4
HEARING DATE:	July 14, 2022
CASE NUMBER:	<u>ZAP1518MA22 – BCI IV Harvill Industrial Center</u> (Representative: EPD Solutions)
APPROVING JURISDICTION:	County of Riverside
JURISDICTION CASE NO:	PPT220002 (Plot Plan)
LAND USE PLAN:	2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan
Airport Influence Area:	March Air Reserve Base
Land Use Policy:	Zones C2
Noise Levels:	Below 60 CNEL contour
MAJOR ISSUES:	None

RECOMMENDATION: Staff recommends that the Commission find the proposed Plot Plan **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, subject to the conditions included herein.

PROJECT DESCRIPTION: A proposal to construct a 434,823 square foot manufacturing building with mezzanines on 20.57 acres. The applicant also proposes to construct a rooftop solar panel system on the proposed building.

PROJECT LOCATION: The site is located northerly of Orange Avenue, westerly of Harvill Avenue, easterly of Tobacco Road, and southerly of Water Street, approximately 16,708 feet southwesterly of the southerly end of Runway 14-32 at March Air Reserve Base.

BACKGROUND:

Non-Residential Average Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zone C2, where Zone C2 limits average intensity to 200 people per acre.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan and the Additional Compatibility Policies included in the March ALUCP, the following rates were used to calculate the occupancy for the proposed project:

- Manufacturing – 1 person per 200 square feet, and

- Office – 1 person per 200 square feet.

The project proposes to construct a 434,823 square foot manufacturing building, which includes 424,823 square feet of manufacturing area, 5,000 square feet of first floor office area, and 5,000 square feet of second floor office mezzanine area, accommodating a total occupancy of 2,174 people, resulting in an average intensity of 106 people per acre, which is consistent with Zone C2 average intensity criterion of 200 people per acre.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle and 1.0 persons per trailer truck space). Based on the number of parking spaces provided (254 standard vehicles, 66 trailer spaces), the total occupancy would be estimated at 447 people for an average intensity of 22 people per acre, which is consistent with the Compatibility Zone C2 average intensity criterion.

Non-Residential Single-Acre Intensity: Compatibility Zone C2 limits maximum single-acre intensity to 500 people. There are no risk-reduction design bonuses available, as March Air Reserve Base/Inland Port Airport is primarily utilized by large aircraft weighing more than 12,500 pounds.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre area would include 38,560 square feet of manufacturing area, 5,000 square feet of first floor office area, and 5,000 square feet of second floor office mezzanines, resulting in a single acre occupancy of 243 people which is consistent with the Compatibility Zone C2 single acre criterion of 500.

March Air Reserve Base/United States Air Force Input: Given that the project site is located in Zone C2 westerly of the southerly runway at March Air Reserve Base, the March Air Reserve Base staff was notified of the proposal to add rooftop solar panels, and sent a solar glare hazard analysis study for their review. On June 6, 2022, the Air Force provided comments concurring with the analysis and conclusions of the glare study, with the added note that their general concerns regarding cumulative impacts of solar projects are being addressed in the ongoing March Compatibility Use Study update.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zones C2 (children's schools, day care centers, hospitals, nursing homes, libraries, places of assembly, highly noise-sensitive outdoor nonresidential uses and hazards to flight).

Flight Hazard Issues: Structure height, electrical interference, and reflectivity/glare are among the issues that solar panels in the airport influence area must address. The project's photovoltaic (PV) panel structures would be located on the rooftop of the proposed industrial building within Compatibility Zone C2.

Glint and Glare/Reflectivity

Based on the Federal Aviation Administration's Interim Policy for Review of Solar Energy System Projects on Federally Obligated Airports, no glare potential or low potential for temporary after-image ("green" level) are acceptable levels of glare on final approach (within 2 miles from end of runway) for solar facilities located on airport property. However, potential for temporary after-image ("yellow" level) and potential for permanent eye damage ("red" level) are not acceptable levels of

glare on final approach. No glare is permitted at air traffic control towers.

The project proposes solar panels on the proposed building rooftop with a fixed tilt of 34 degrees with no rotation, and an orientation of 180 degrees. The applicant has submitted a glare analysis utilizing the web-based Forge Solar, a copy of which is attached hereto. The analysis was based on a 2 mile straight in approach (as per FAA Interim Policy standards) to runways 14 and 32, and also based on the traffic patterns as identified by March Air Reserve Base staff (Runway 12/30 General Aviation, Runway 14/32 General Aviation, Runway 14/32 C-17/KC-135, Runway 14/32 Overhead). The analysis utilized a glide slope approach of 3.0 degrees. No glare would affect the Air Traffic Control Tower.

The analysis concluded that no glare would occur on the 2 mile approach to the runways, and some potential for glare was identified within the Air Force traffic pattern. Evaluation of the Air Force traffic patterns indicates that the panels would result in a low potential for temporary after-image ("green" level glare) or no glare. All times are in standard time.

- Runway 14/32 C-17/KC-135 traffic pattern totaling 51,939 minutes of 'green' level glare, lasting up to 300 minutes a day, throughout the year, from 6:00 a.m. to 6:00 p.m.
- Runway 14/32 Overhead Aviation traffic pattern totaling 9,708 minutes of 'green' level glare lasting up to 50 minutes a day, throughout the year, from 7:00 a.m. to 6:00 p.m.

The total of 61,647 minutes of "green" level glare represents less than 23 percent of total day light time.

Electrical and Communication Interference

The applicant has indicated that they do not plan to utilize equipment that would interfere with aircraft communications. The PV panels themselves present little risk of interfering with radar transmission due to their low profiles. In addition, solar panels do not emit electromagnetic waves over distances that could interfere with radar signal transmissions, and any electrical facilities that do carry concentrated current will be buried beneath the ground and away from any signal transmission. There are no radar transmission or receiving facilities within the site.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being below the 60 CNEL range from aircraft noise. Therefore, no special measures are required to mitigate aircraft-generated noise.

Part 77: The elevation of Runway 14-32 at its southerly terminus is 1,488 feet above mean sea level (AMSL). At a distance of approximately 16,708 feet from the project to the nearest point on the runway, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,655 feet AMSL. The site's finished floor elevation is 1,538 feet AMSL and proposed building height is 52 feet, resulting in a top point elevation of 1,590 feet AMSL. Therefore, review of the building for height/elevation reasons by the FAA Obstruction Evaluation Service (FAAOES) is not required.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33C). The project is located 8,948 feet from the runway, and therefore would be subject to the above requirement.

The project includes a 20,983 square foot bioretention basin. Bioretention basins are not recommended in the vicinity of airports due to the potential that such areas could provide food, water, and shelter for hazardous wildlife. Pursuant to the study "Wildlife Hazard Management at Riverside County Airports: Background and Policy", October 2018, by Mead & Hunt, which is the basis of the brochure titled "Airports, Wildlife and Stormwater Management", such limited basins are permissible with the appropriate criteria: the basin is used in conjunction with appropriate landscaping for such uses as adjacent to structures, parking islands, medians, site entrances, planter boxes, and vegetation is selected carefully so as not to provide food, shelter, nesting, roosting, or water for wildlife. The project has been conditioned to be consistent with the basin criteria (as well as providing 48-hour draw down of the basin).

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight or circling climb following takeoff or toward an aircraft engaged in a straight or circling final approach toward a landing at an airport, other than a DoD or FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight or circling climb following takeoff or towards an aircraft engaged in a straight or circling final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.

- (e) Highly noise-sensitive outdoor nonresidential uses. Examples of noise-sensitive outdoor nonresidential uses that are prohibited include, but are not limited to, major spectator-oriented sports stadiums, amphitheaters, concert halls and drive-in theaters.
 - (f) Other Hazards to flight.
3. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property, and be recorded as a deed notice.
 4. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

5. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
6. The project has been evaluated to construct 434,823 square foot warehouse building, which includes 424,823 square feet of warehouse area, 5,000 square feet of first floor office area, and 5,000 square feet of second floor office mezzanine area. Any increase in building area, change in use to any higher intensity use, change in building location, or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.
7. All solar arrays installed on the project site shall consist of smooth glass photovoltaic solar panels without anti-reflective coating, a fixed tilt of 34 degrees and orientation of 180 degrees. Solar panels shall be limited to a total of 344,124 square feet, and the locations

and coordinates shall be as specified in the glare study. Any deviation from these specifications (other than reduction in square footage of panels), including change in orientation, shall require a new solar glare analysis to ensure that the amended project does not result in any glare impacting the air traffic control tower or creation of any "yellow" or "red" level glare in the flight paths, and shall require a new hearing by the Airport Land Use Commission.

8. In the event that any glint, glare, or flash affecting the safety of air navigation occurs as a result of project operation, upon notification to the airport operator of an event, the airport operator shall notify the project operator in writing. Within 30 days of written notice, the project operator shall be required to promptly take all measures necessary to eliminate such glint, glare, or flash. An "event" includes any situation that results in an accident, incident, "near-miss," or specific safety complaint regarding an in-flight experience to the airport operator or to federal, state, or county authorities responsible for the safety of air navigation. The project operator shall work with the airport operator to prevent recurrence of the incidence. Suggested measures may include, but are not limited to, changing the orientation and/or tilt of the source, covering the source at the time of day when events of glare occur, or wholly removing the source to diminish or eliminate the source of the glint, glare, or flash. For each such event made known to the project operator, the necessary remediation shall only be considered to have been fulfilled when the airport operator states in writing that the situation has been remediated to the airport operator's satisfaction.
9. In the event that any electrical interference affecting the safety of air navigation occurs as a result of project operation, upon notification to the airport operator of an event, the airport operator shall notify the project operator in writing. Within 30 days of written notice, the project operator shall be required to promptly take all measures necessary to eliminate such interference. An "event" includes any situation that results in an accident, incident, "near-miss," report by airport personnel, or specific safety complaint to the airport operator or to federal, state, or county authorities responsible for the safety of air navigation. The project operator shall work with the airport operator to prevent recurrence of the event. For each such event made known to the project operator, the necessary remediation shall only be considered to have been fulfilled when the airport operator states in writing that the situation has been remediated to the airport operator's satisfaction.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

**THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS**

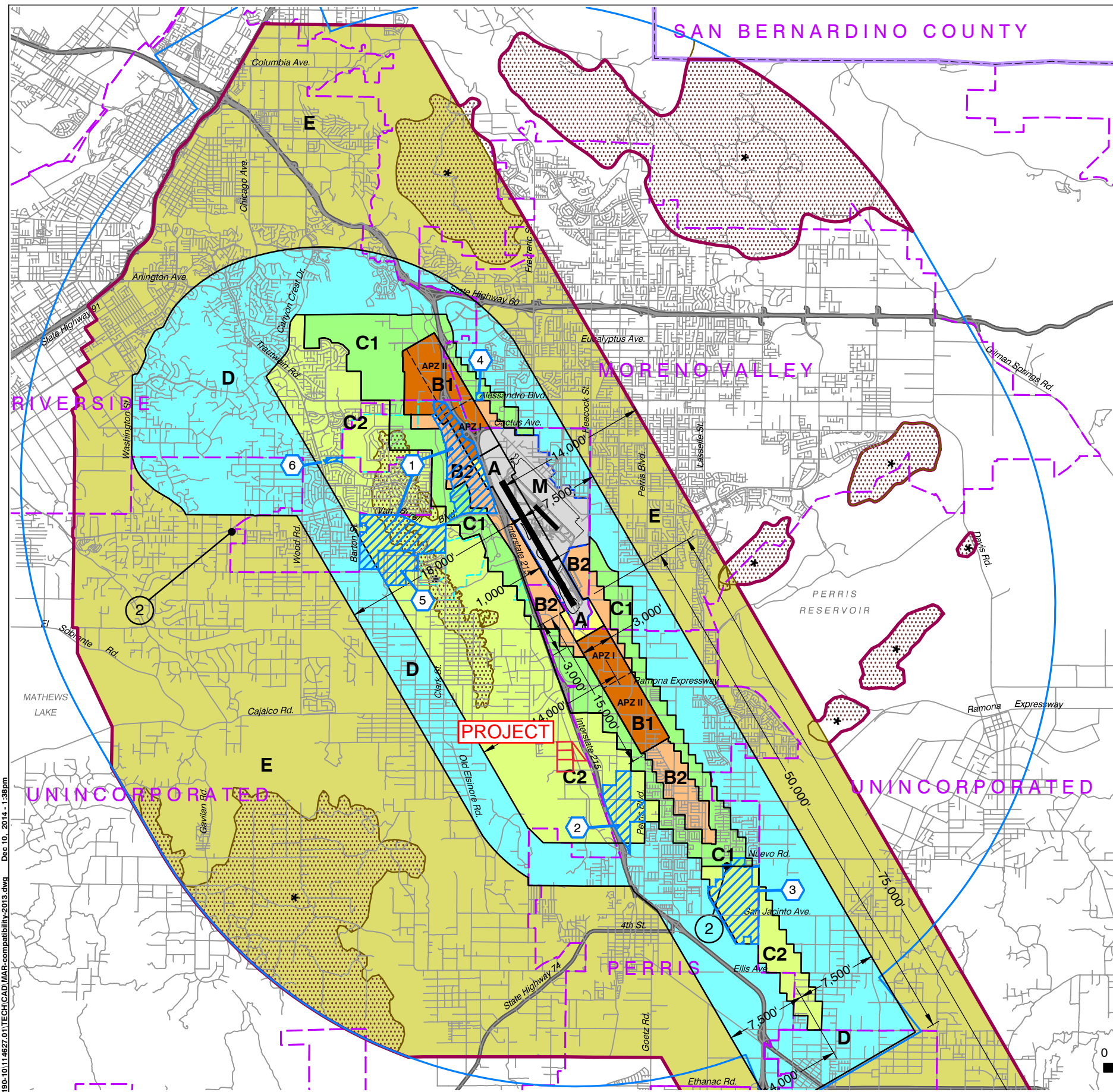
**PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES**



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



LEGEND

Compatibility Zones

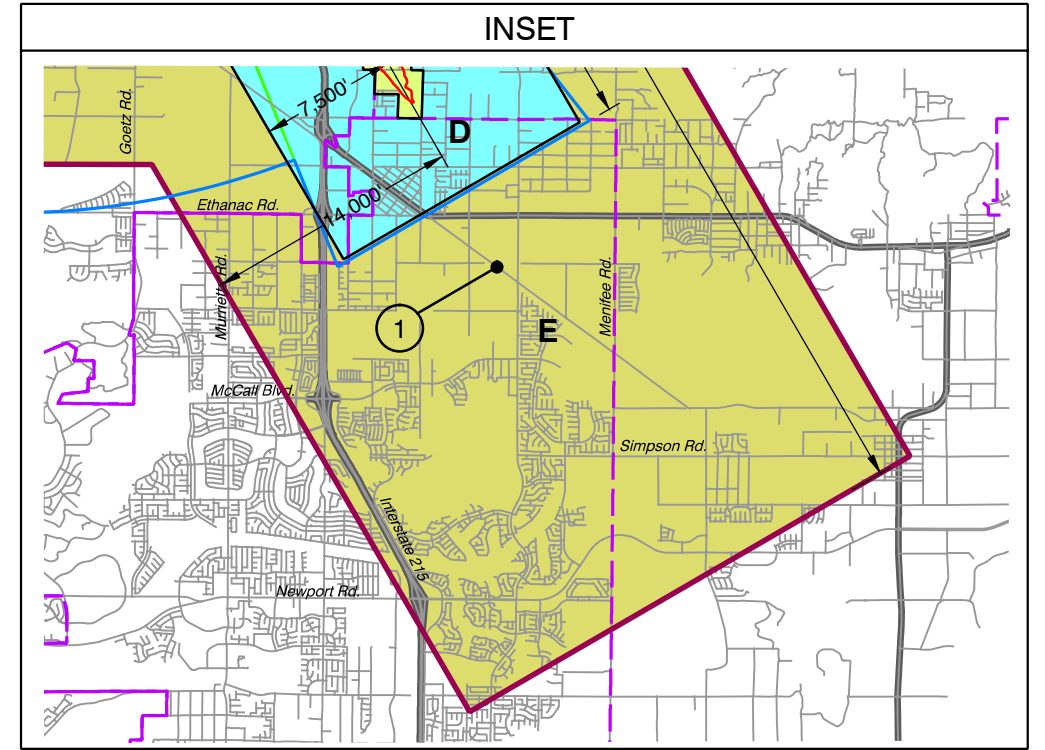
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

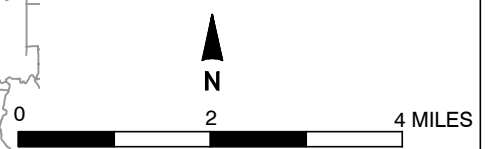
- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



**Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)**

Note:
All dimensions are measured from runway ends and centerlines.



Base map source: County of Riverside 2013

SEE INSET AT RIGHT

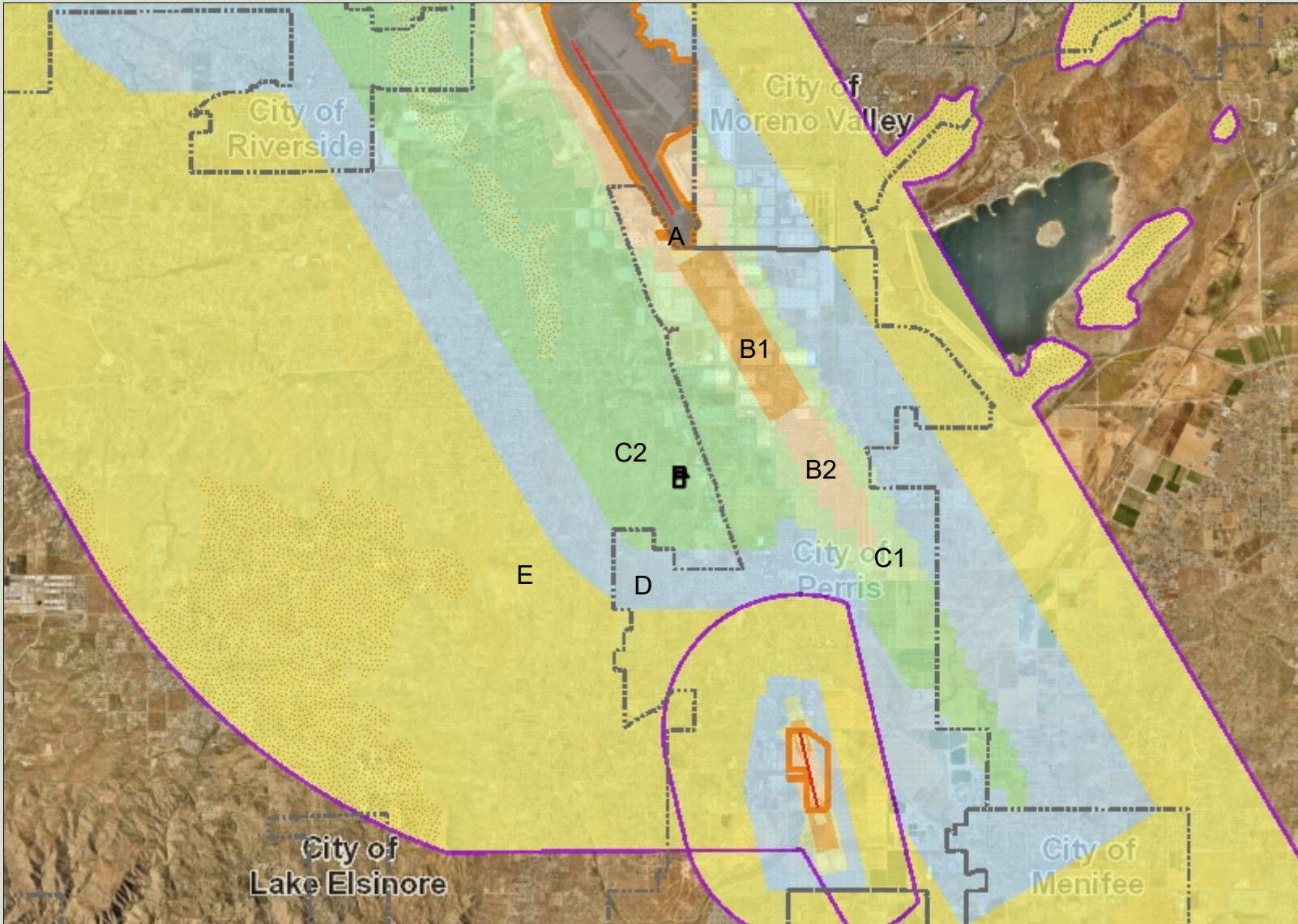
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Prepared by Mead & Hunt, Inc. (June 2013)

Map MA-1

**Compatibility Map
March Air Reserve Base / Inland Port Airport**

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

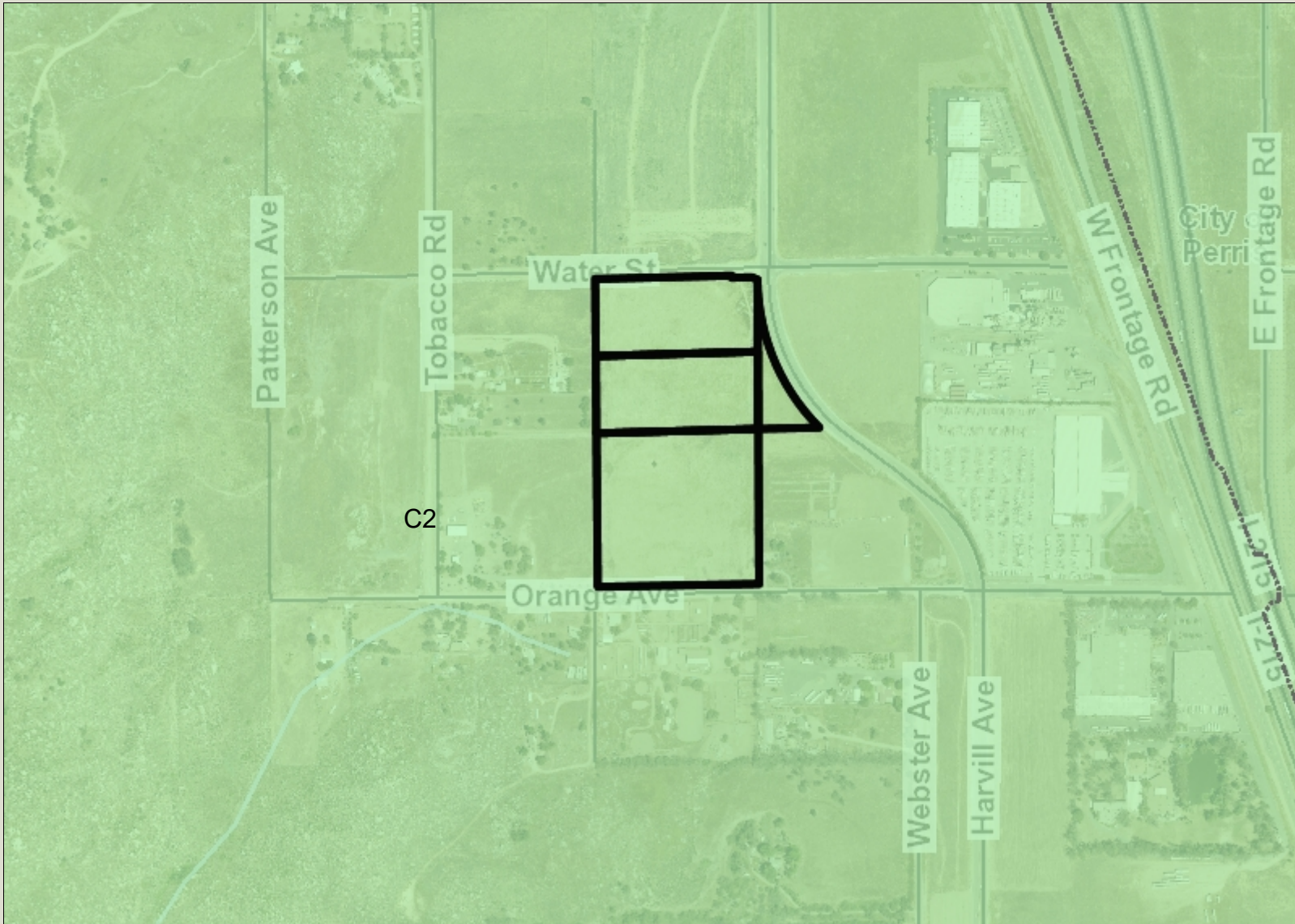


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Notes

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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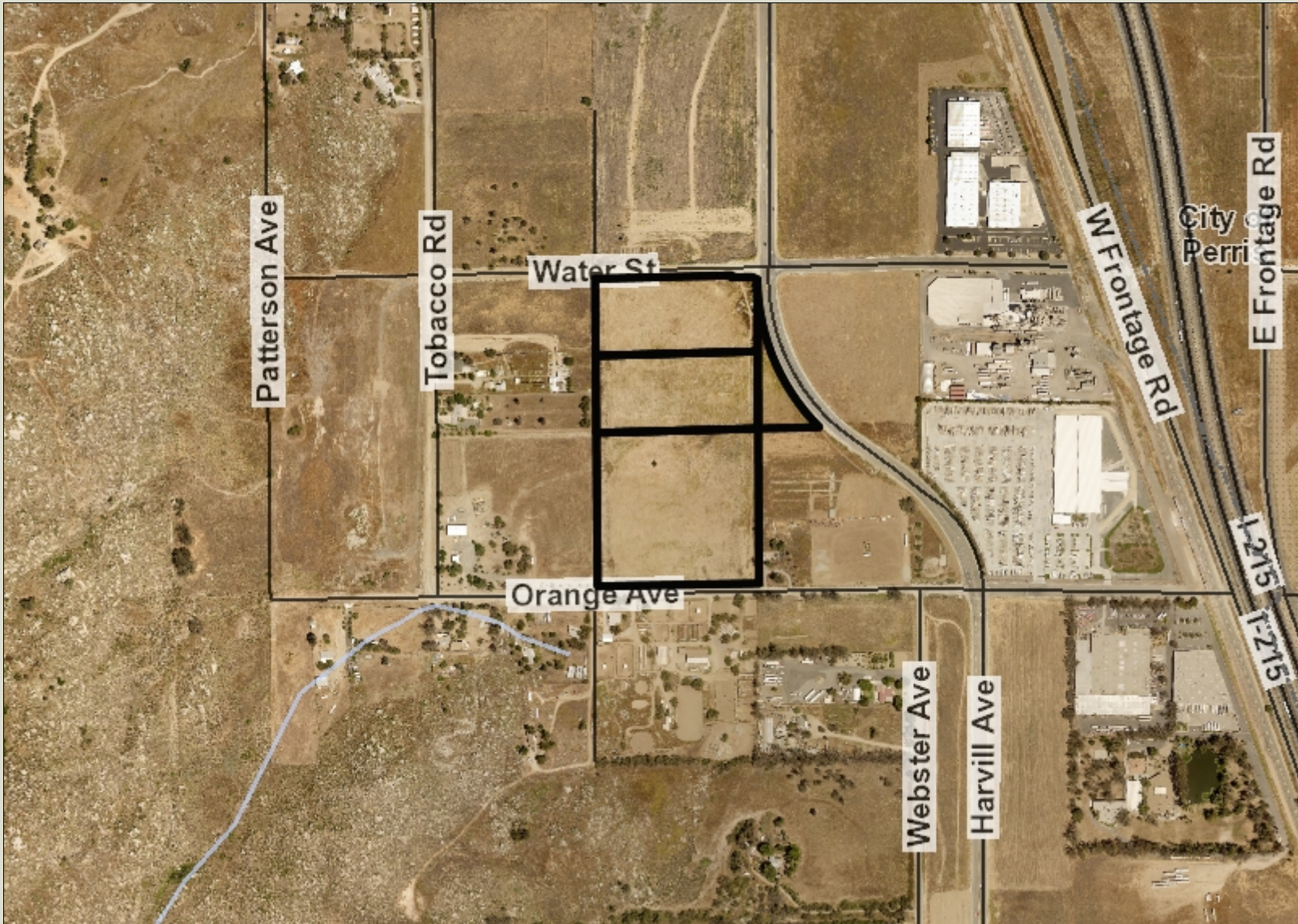
0 770 1,539 Feet

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Notes

Map My County Map



- Legend**
- County Centerline Names
 - County Centerlines
 - Blueline Streams
 - City Areas
 - World Street Map



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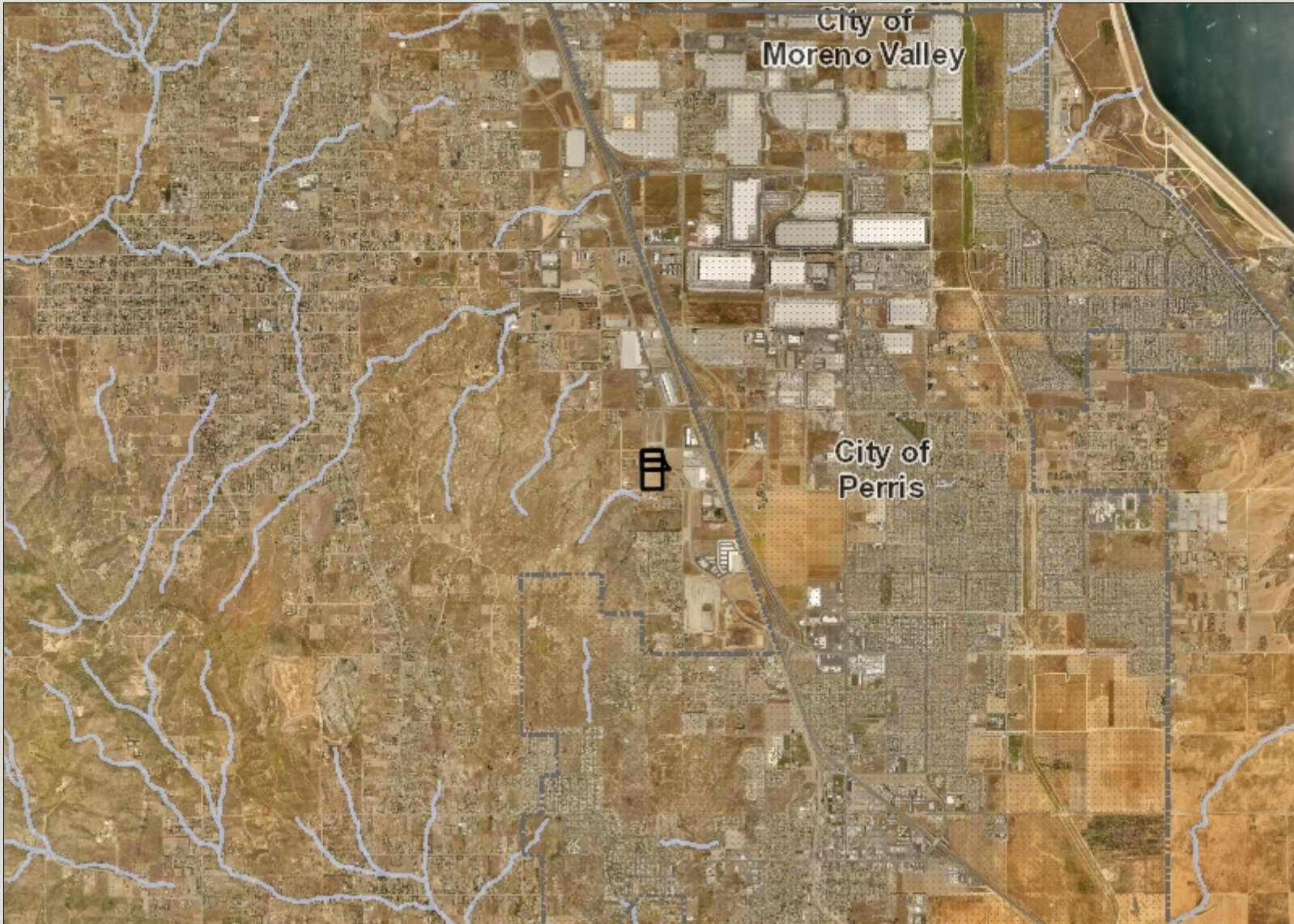


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Notes

Map My County Map



Legend

-  Blueline Streams
-  City Areas
-  World Street Map



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Notes



Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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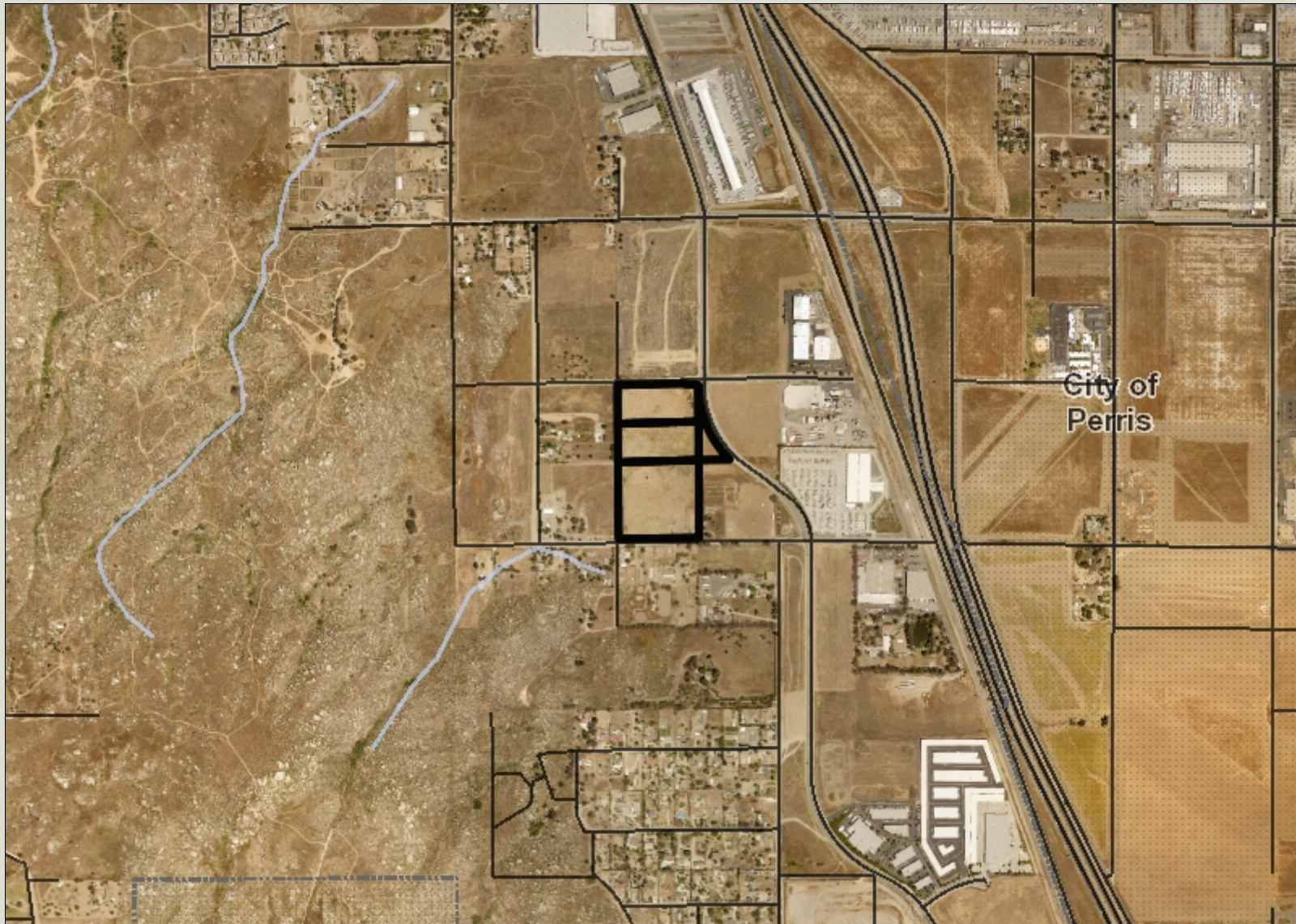
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Map My County Map



Legend

- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



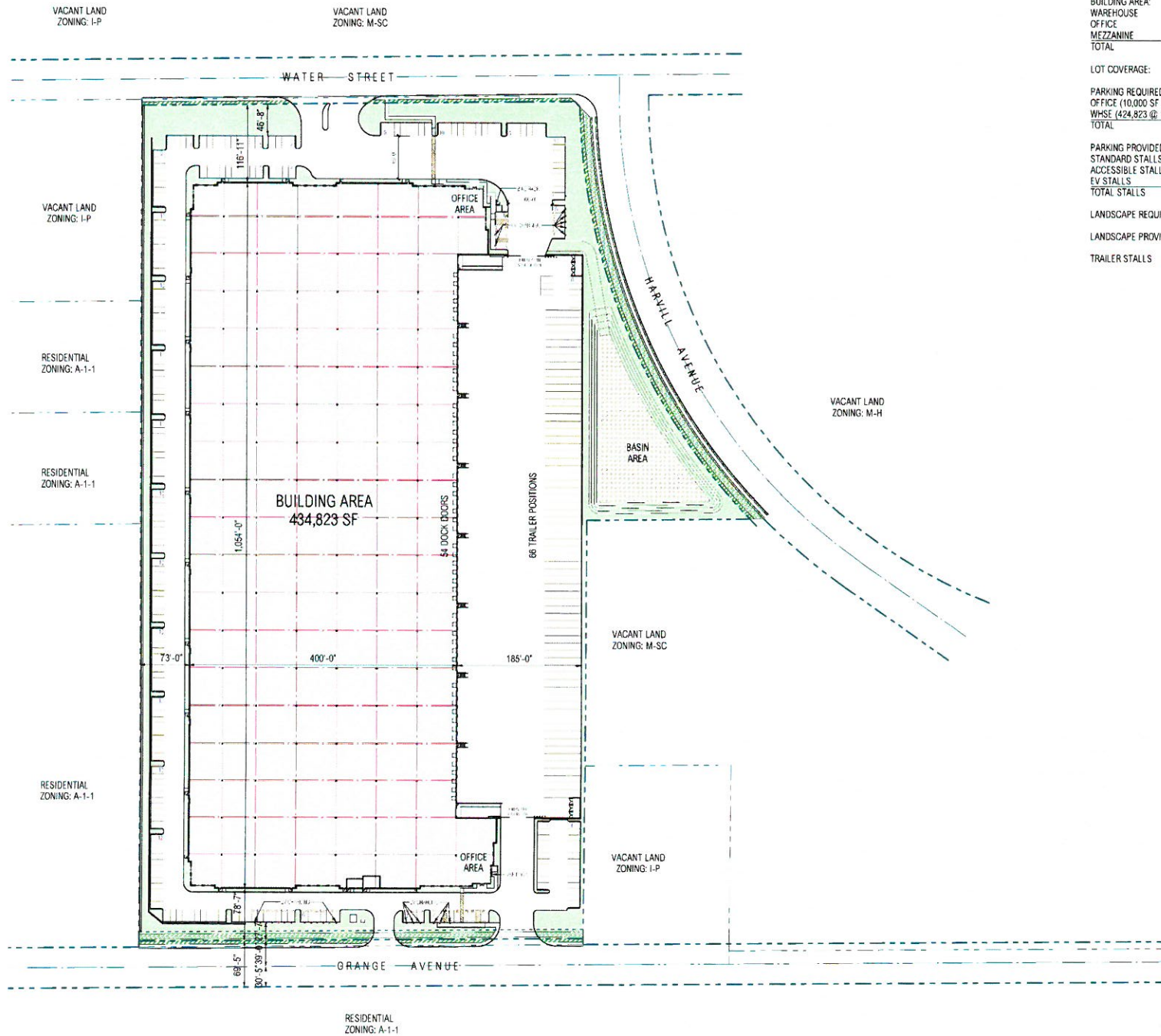
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Notes



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PROJECT DATA

NET SITE AREA	874,685 SF / 20.08 AC
BUILDING AREA:	
WAREHOUSE	424,823 SF
OFFICE	5,000 SF
MEZZANINE	5,000 SF
TOTAL	434,823 SF
LOT COVERAGE:	49.71 %
PARKING REQUIRED:	
OFFICE (10,000 SF @ 1/250 SF)	40 STALLS
WHSE (424,823 @ 1/2,000 SF)	213 STALLS
TOTAL	253 STALLS
PARKING PROVIDED:	
STANDARD STALLS	222 STALLS
ACCESSIBLE STALLS	6 STALLS
EV STALLS	26 STALLS
TOTAL STALLS	254 STALLS
LANDSCAPE REQUIRED:	131,203 SF / 15.0 %
LANDSCAPE PROVIDED:	135,180 SF / 15.45 %
TRAILER STALLS	66 TRAILERS



HARVILL AVE
INDUSTRIAL WAREHOUSE
PROJECT
PPT220002
HARVILL AVENUE
COUNTY OF RIVERSIDE
CALIFORNIA

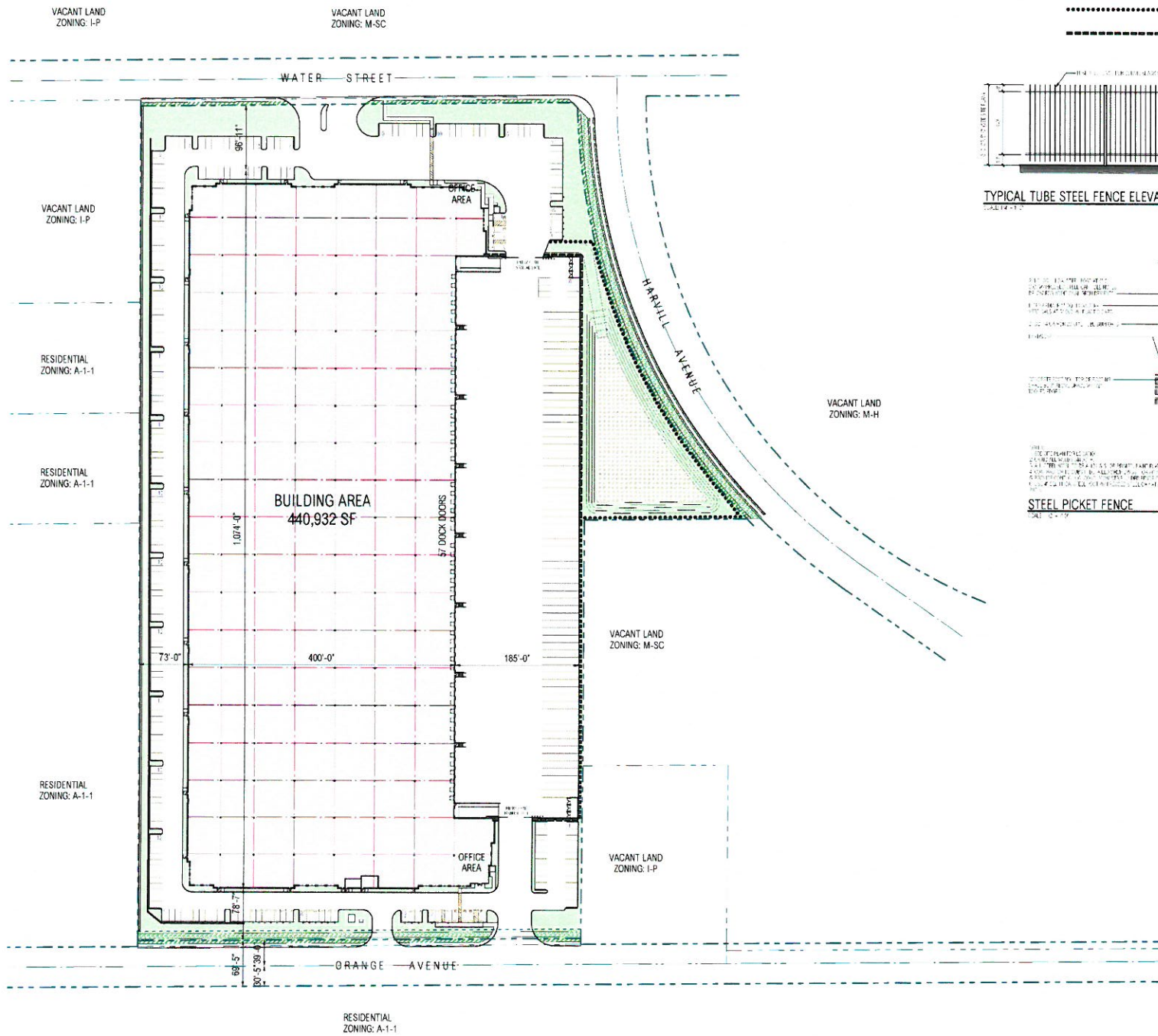
APPLICANT:
ROA OFFICE OF ARCHITECTURAL DESIGN
15231 ALTON PARKWAY SUITE 100
NEWPORT BEACH, CA 92660
TEL: 949.441.0000
CONTACT: JACOB WEBER
jacob@roaarchitect.com

DEVELOPER:
ROA HARVILL BUSINESS CENTER, 37
6505 HARVILL BUSINESS CENTER, SUITE 200
NEWPORT BEACH, CA 92660
TEL: 949.441.0000
CONTACT: JACOB WEBER
jacob@roaarchitect.com

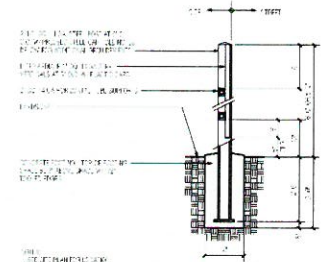
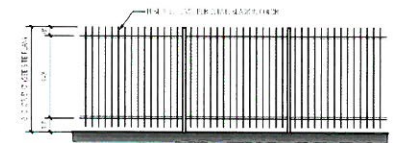
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100	10/15/2021	ISSUED FOR PERMITTING

SITE PLAN





SCREENING LEGEND



FENCE PLAN

RG
Office of Architectural
15231 Alton Parkway, Suite 100
Irvine, CA
T 949-261-1349
F 949-261-1348

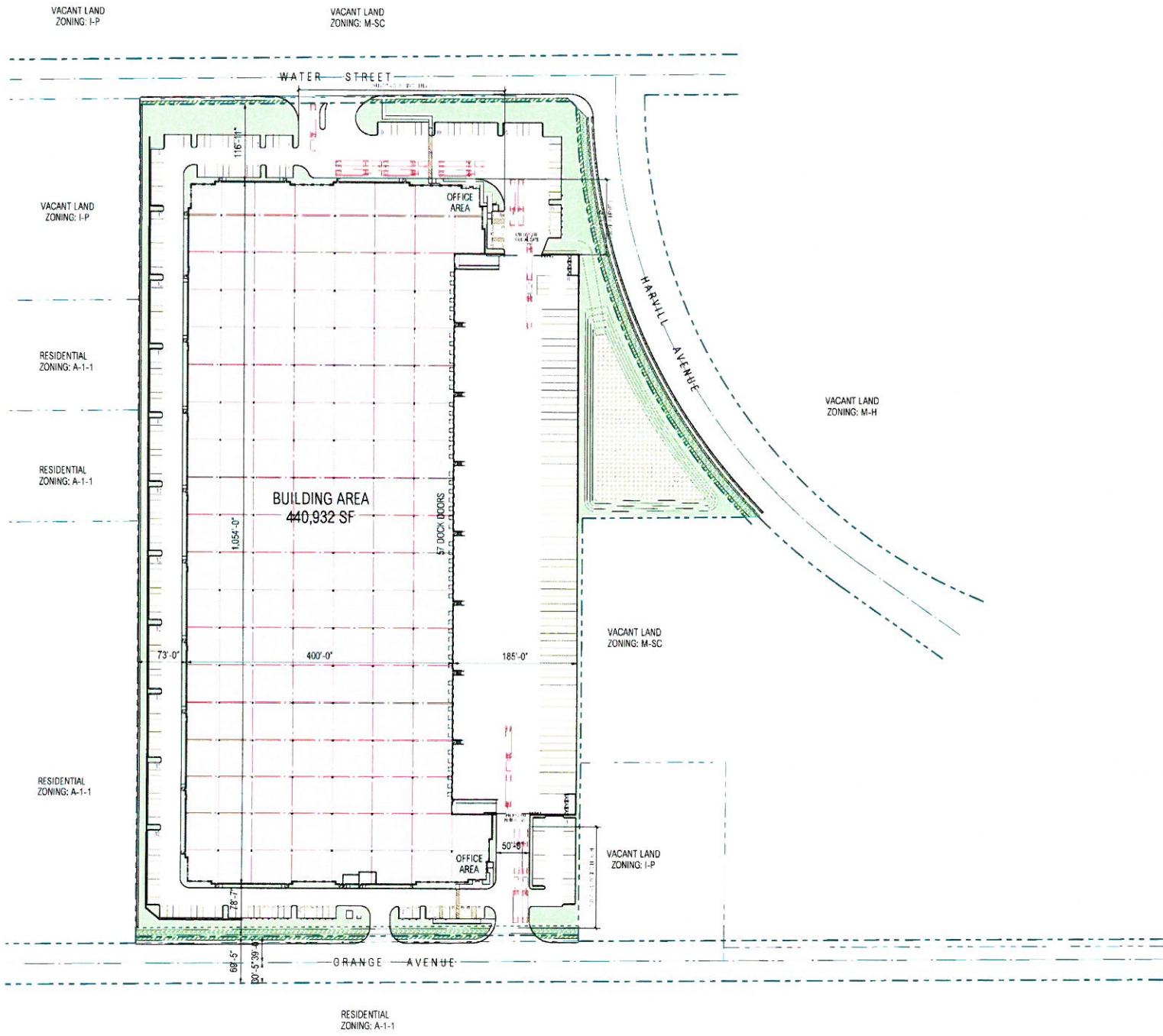
PROJECT: PPT2002
HARVILL AVE.
INDUSTRIAL WAREHOUSE
PROJECT
PPT220002
HARVILL AVENUE,
COUNTY OF RIVERSIDE,
CALIFORNIA

APPLICANT:
RGA OFFICE OF ARCHITECTURAL DESIGN
15231 ALTON PARKWAY SUITE 100
IRVINE, CALIFORNIA 92618
T: 949-261-1349
CONTACT: JACOB HEEPER
jacob@rga-architect.com

DEVELOPER:
BCT HARVILL BUSINESS CENTER, LLC
7615 MACARTHUR COURT SUITE 625
NEWPORT BEACH, CA 92660
TEL: 949-882-4300
CONTACT: PETER H. SCHAEFER
peter.schaefer@bctharvill.com

NO.	DATE	DESCRIPTION
1	01/15/2022	ISSUED FOR PERMIT
2	02/01/2022	REVISED PER PERMIT COMMENTS
3	02/15/2022	REVISED PER PERMIT COMMENTS
4	02/25/2022	REVISED PER PERMIT COMMENTS
5	03/01/2022	REVISED PER PERMIT COMMENTS
6	03/15/2022	REVISED PER PERMIT COMMENTS
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8	04/01/2022	REVISED PER PERMIT COMMENTS
9	04/15/2022	REVISED PER PERMIT COMMENTS
10	04/25/2022	REVISED PER PERMIT COMMENTS
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12	05/15/2022	REVISED PER PERMIT COMMENTS
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99	10/15/2024	REVISED PER PERMIT COMMENTS
100	10/25/2024	REVISED PER PERMIT COMMENTS

FENCE AND WALL PLAN



DATE: 04/11/2023

PROJECT: INDUSTRIAL WAREHOUSE

HARVILL AVE
 INDUSTRIAL WAREHOUSE
 PROJECT
 PPT220002
 HARVILL AVENUE
 COUNTY OF RIVERS
 CALIFORNIA

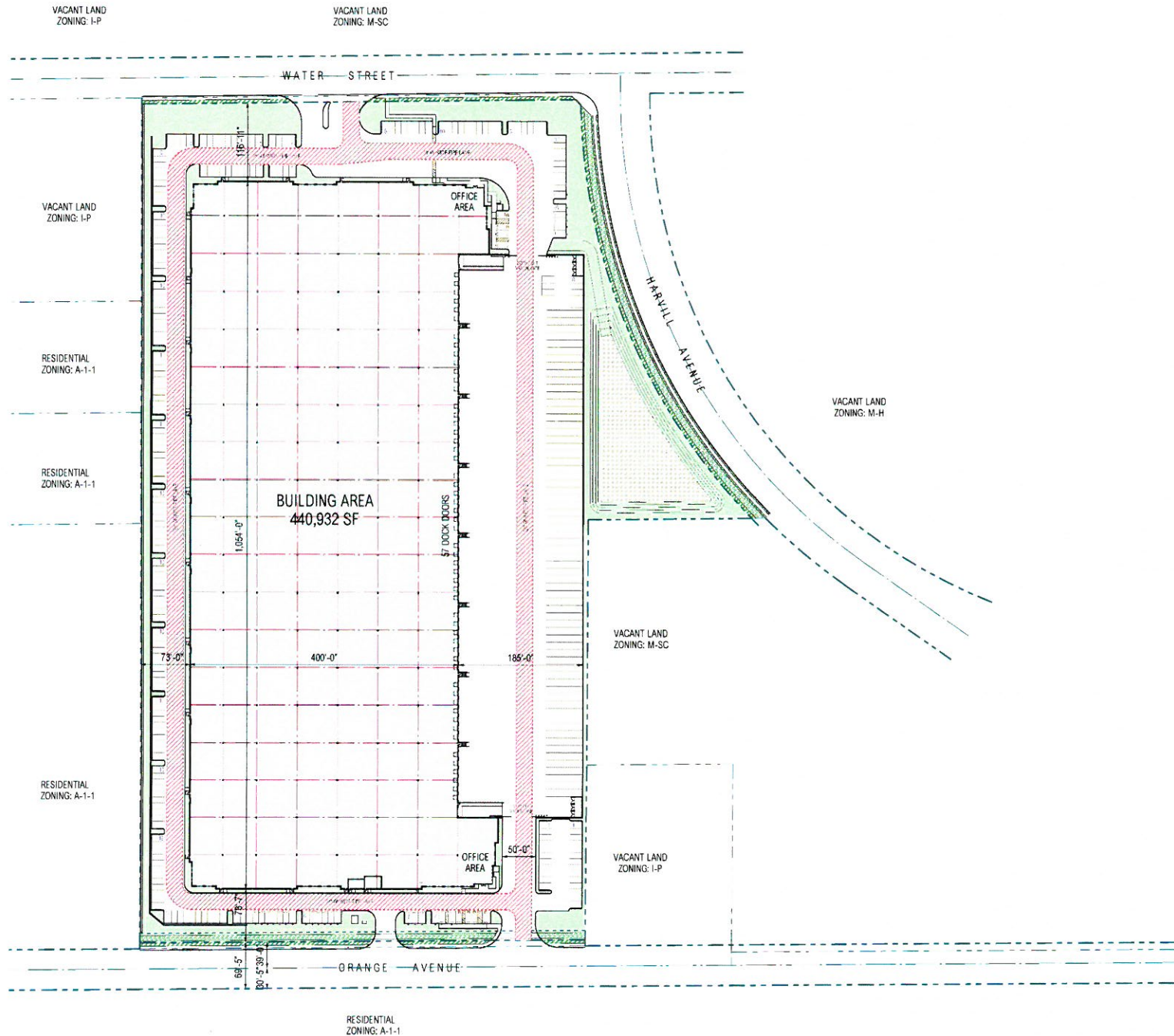
APPLICANT:
 BOARD OF ARCHITECTURAL REVIEW
 17231 ALTON PARKWAY SUITE 100
 IRVINE, CA 92618
 T 949-451-1000
 CONTACT: JACOB WEBER
 jacob@rg-arch.com

DEVELOPER:
 8017 HARVILL BUSINESS CENTER I/P
 4375 MACARTHUR COURT SUITE 603
 NEWPORT BEACH, CA 92660
 TEL: 949 802 4000
 CONTACT: TERRY SCHAEFER
 terry.schaefer@blackstoneip.com

NO.	DATE	DESCRIPTION
01	04/11/2023	ISSUED FOR PERMIT
02	04/11/2023	ISSUED FOR PERMIT
03	04/11/2023	ISSUED FOR PERMIT
04	04/11/2023	ISSUED FOR PERMIT
05	04/11/2023	ISSUED FOR PERMIT
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08	04/11/2023	ISSUED FOR PERMIT
09	04/11/2023	ISSUED FOR PERMIT
10	04/11/2023	ISSUED FOR PERMIT

FENCE PLAN

TRUCK STAGING EXHIBIT



FENCE PLAN

DATE	07/15/14
PROJECT	Harvill Ave Industrial Warehouse Project
CLIENT	Harvill Avenue Industrial Warehouse Project
LOCATION	Harvill Avenue, County of Riverside, California
SCALE	AS SHOWN

DATE	07/15/14
PROJECT	Harvill Ave Industrial Warehouse Project
CLIENT	Harvill Avenue Industrial Warehouse Project
LOCATION	Harvill Avenue, County of Riverside, California
SCALE	AS SHOWN

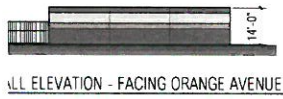
HARVILL AVE INDUSTRIAL WAREHOUSE PROJECT
 PPT220002
 HARVILL AVENUE
 COUNTY OF RIVERSIDE
 CALIFORNIA

APPLICANT:
 RGA OFFICE OF ARCHITECTURAL DESIGN
 15231 ALTON PARKWAY SUITE 100
 IRVINE, CA 92618
 T: 949.341.0800
 CONTACT: JACOB WEBER
 jacob@rgaarchitect.com

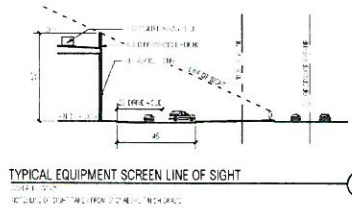
DEVELOPER:
 8511 HARVILL BUSINESS CENTER, LOT 2
 7675 MACARTHUR COURT, SUITE 623
 NEWPORT BEACH, CA 92660
 TEL: 949.482.4899
 CONTACT: PHILIP SCHAEFER
 philip.schaefer@blackmergroup.com

NO.	DATE	DESCRIPTION
1	07/15/14	PRELIMINARY
2	07/15/14	FINAL
3	07/15/14	AS SHOWN
4	07/15/14	AS SHOWN
5	07/15/14	AS SHOWN
6	07/15/14	AS SHOWN
7	07/15/14	AS SHOWN
8	07/15/14	AS SHOWN
9	07/15/14	AS SHOWN
10	07/15/14	AS SHOWN

PRELIMINARY EXHIBIT



WEST ELEVATION - FACING ORANGE AVENUE



TYPICAL EQUIPMENT SCREEN LINE OF SIGHT
SEE ELEVATION 1
SECTION OF DISTANCE FROM EQUIPMENT TO SCREEN

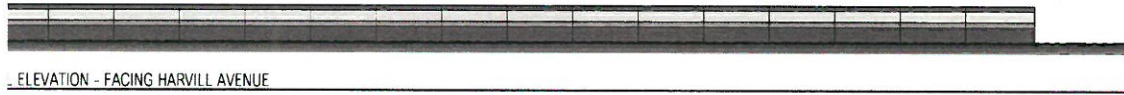
FINISH SCHEDULE

P-1	MCC-1
P-2	M-1
P-3	GL-1
P-4	

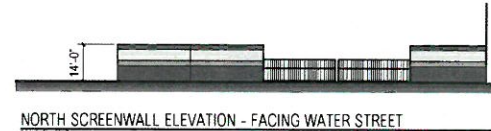
FINISH SCHEDULE	
1	CONCRETE
2	PAINT
3	GLASS
4	WOOD
5	STEEL
6	BRICK
7	STONE
8	ROOFING
9	MECHANICAL
10	ELECTRICAL
11	PLUMBING
12	MECHANICAL
13	ELECTRICAL
14	PLUMBING
15	MECHANICAL
16	ELECTRICAL
17	PLUMBING
18	MECHANICAL
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20	PLUMBING
21	MECHANICAL
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28	ELECTRICAL
29	PLUMBING
30	MECHANICAL

KEYNOTES

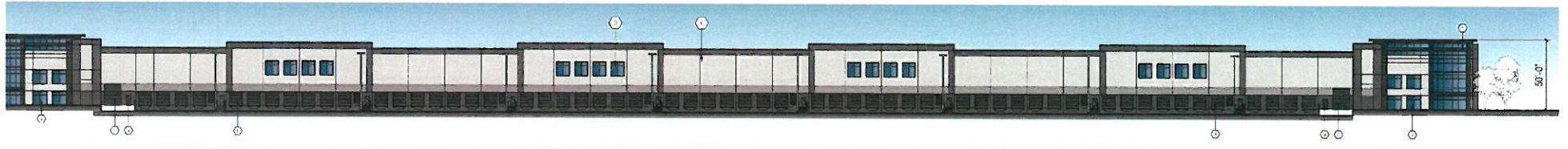
1. ALL DIMENSIONS ARE IN FEET AND INCHES.
2. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED.
3. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED.
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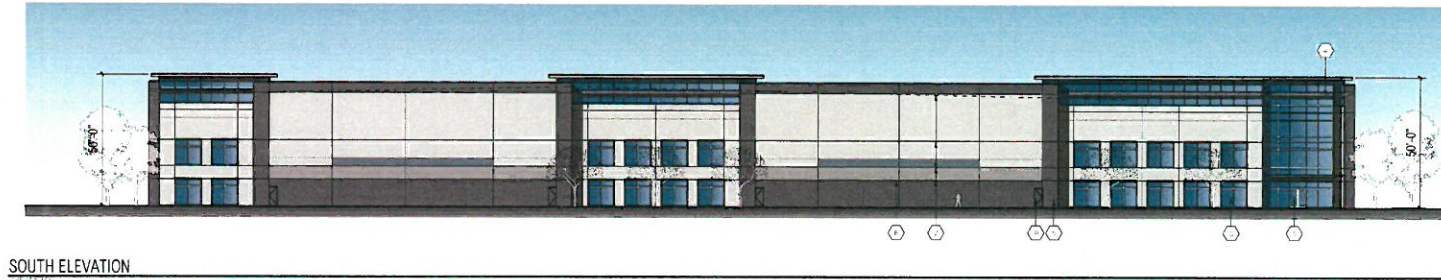
ELEVATION - FACING HARVILL AVENUE



NORTH SCREENWALL ELEVATION - FACING WATER STREET



SOUTH ELEVATION



NORTH ELEVATION



ENVIRONMENT | PLANNING | DEVELOPMENT SOLUTIONS, INC.

Date: April 26, 2022
Prepared by: Abby Pal
To: Michael Brown, Project Manager
Site: Harvill Avenue and Water Street Warehouse Project
Subject: Rooftop Solar Panel Installation Glare Analysis

This technical memorandum evaluates the potential solar glare resulting from the installation of solar panels on the roof of the proposed 434,823 square feet warehouse at the southwest corner of the intersection of Harvill Avenue and Water Street in the City of Perris. The project is within the March Air Reserve Base (March ARB) airport influence area. The solar glare analysis assesses the compatibility of the proposed solar panels as per the requirements of March ARB. The analysis and findings of this memo are intended for review and acceptance by Riverside County, Riverside County Airport Land Use Commission (ALUC) and the March ARB.

Project Description

As mentioned above, the proposed solar panel installation would be located at the southwest corner of the intersection of Harvill Avenue and Water Street in the City of Perris. The location of the project with respect to March ARB is shown in *Attachment A*. The site plan of the proposed warehouse project is shown in *Attachment B*. The solar glare analysis assumes 80% roof coverage by the solar panel installation which would be approximately 344,124 square feet in area. The rooftop height for the proposed warehouse development is 49 feet. The solar panel installation was assumed to be smooth glass with anti-reflective coating, fixed mounted at 3 feet above the ground with a tilt of 34.0° and orientated at 180.0°.

Solar Glare Analysis Tools and Methodology

The potential impact of glint and glare from photovoltaic modules, concentrating solar collectors, receivers, and other components has received increased attention as a potential hazard or distraction for pilots, air-traffic control and other airport personnel. Hazards from reflected solar radiation include the potential for permanent eye injury (e.g., retinal burn from concentrated sunlight) and temporary disability or distractions (e.g., glint, glare, after-images). The Federal Aviation Administration (FAA) requires the sponsor of proposed construction or alteration to confirm that the potential for glint and glare has been analyzed and determined to have no potential for ocular impact to the airport's Air Traffic Control Tower (ATCT) cab¹.

Sandia National Laboratories (National Technology and Engineering Solutions of Sandia, LLC.) developed early Solar Glare Hazard Analysis Tools (SGHAT) which included programs for modeling and analyzing potential hazards from solar glare. These tools and programs had been adopted as a standard for FAA and other airport/user reviews. Due to new cybersecurity restrictions at Sandia, SGHAT is now available for internal Sandia use only. All external use of SGHAT is restricted, however the glare tool source code and algorithms were made available for licensing. The FAA policy to demonstrate compliance with 14 CFR 77.5 (c)² updated on May 11, 2021 withdrew their recommendation of the Solar Glare Hazard Analysis Tool (SGHAT) to analyze ocular impact as the tool is no longer available to all users at no cost. However, EPD obtained the use of ForgeSolar Glare Analysis tool which utilizes the SGHAT glare tool source code and

¹ Federal Register. Referenced at <https://www.federalregister.gov/documents/2021/05/11/2021-09862/federal-aviation-administration-policy-review-of-solar-energy-system-projects-on-federally-obligated>

² Code of Federal Regulations. Referenced at [https://www.ecfr.gov/current/title-14/chapter-I/subchapter-E/part-77/subpart-B/section-77.5#p-77.5\(c\)](https://www.ecfr.gov/current/title-14/chapter-I/subchapter-E/part-77/subpart-B/section-77.5#p-77.5(c))

algorithms under subscription made available by Sims Industries (d/b/a ForgeSolar) offering comparable tools for FAA-certifiable glare analysis.

This solar glare analysis memo incorporates the following:

- 1) Potential for glint or glare in the existing or planned Airport Traffic Control Tower (ATCT) cab.
- 2) Potential for glare or "low potential for after-image" along the final approach path for any existing landing threshold or future landing thresholds. The final approach path is defined as 2 miles from 50 feet above the landing threshold using a standard 3° glidepath.
- 3) Ocular impact analyzed over the entire calendar year in one-minute intervals from when the sun rises above the horizon until the sun sets below the horizon.

In addition to the above, this memo also analyzes a series of air traffic patterns for the two existing runways at March ARB established by the Riverside County ALUC staff in conjunction with March ARB staff. This is to ensure that the land uses around the base are compatible with its air operations and that the proposed solar panel installations will not create a hazard to air navigation as a result of reflected sunlight and the associated potential glare. The following March ARB standard traffic patterns have been included for analysis:

- Runway 12/30 General Aviation Traffic Pattern
- Runway 14/32 General Aviation Traffic Pattern
- Runway 14/32 C-17/KC-135 Traffic Pattern
- Runway 14/32 Overhead Traffic Pattern

Findings

The light reflected from the surface of solar panels can result in glint (a momentary flash of bright light) and glare (a continuous source of bright light). These two effects can cause a brief loss of vision which can hamper the safe maneuvering of the aircraft while in flight. Ocular impact from solar glare can result in green glare or yellow glare. Green glare can be defined as glare with low potential to cause after image or flash blindness for a few seconds which would not hamper safe aircraft maneuvering. Yellow glare can be defined as glare with potential to cause temporary after-image lasting more than a few seconds that might hamper safe aircraft maneuvering. Yellow glare is not acceptable as per glare hazard model criteria and would require mitigation to reduce ocular impact to green glare or better.

As per the solar glare analysis conducted, the proposed rooftop solar panel installation would result in green glare as shown in Table 1 below. No yellow glare is predicted. Therefore, the proposed rooftop solar panel installation would pass the glare hazard model criteria, with zero minutes per year outside the 'green zone' of acceptable reflected solar energy. The glare analysis results for all flight paths and the ocular impact on ATCT is presented in *Attachment C*.

Table 1: Glare Minutes per Year

Analysis Component	Green Glare (min)	Yellow Glare (min)	Pass?
1. Runway 12 Final	0	0	Yes
2. Runway 14 Final	0	0	Yes
3. Runway 30 Final	0	0	Yes
4. Runway 32 Final	0	0	Yes
5. ATCT	0	0	Yes
6. Runway 12-30 General Aviation Traffic Pattern (two-way)	0	0	Yes
7. Runway 14-32 General Aviation Traffic Pattern (two-way)	0	0	Yes
8. Runway 14-32 C-17 KC-135 Rectangular Analysis (two-way)	51939	0	Yes
9. Runway 14 Overhead Analysis (one-way)	0	0	Yes
10. Runway 32 Overhead Analysis (one-way)	9708	0	Yes

If you have any questions about this information, please contact me at (412) 636-2713 or abby@epdsolutions.com.

Attachment A: Solar Panel Location



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I Accept



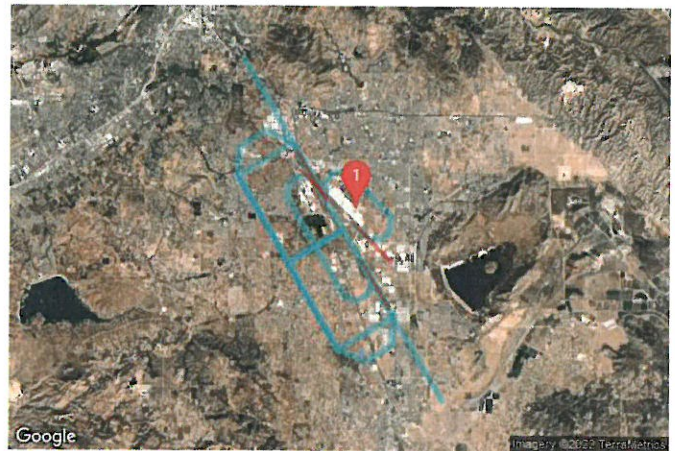
ForgeSolar

March ARB Solar Glare Analysis

21-106 Water and Harvill

Created April 20, 2022
Updated April 21, 2022
Time-step 1 minute
Timezone offset UTC-8
Site ID 67890.11944

Project type Basic
Project status: active
Category 500 kW to 1 MW
(1,000 kW / 8 acre limit)



Misc. Analysis Settings

DNI: varies (1,000.0 W/m² peak)
Ocular transmission coefficient: 0.5
Pupil diameter: 0.002 m
Eye focal length: 0.017 m
Sun subtended angle: 9.3 mrad

Analysis Methodologies:

- Observation point: **Version 2**
- 2-Mile Flight Path: **Version 2**
- Route: **Version 2**

Summary of Results Glare with low potential for temporary after-image predicted

PV Name	Tilt	Orientation	"Green" Glare	"Yellow" Glare	Energy Produced
	deg	deg	min	min	kWh
Harvill Water Solar Panel	34.0	180.0	61,647	0	-

Component Data

PV Array(s)

Total PV footprint area: 7.9 acres

Name: Harvill Water Solar Panel
Description: Roof Top Solar Panel
Footprint area: 7.9 acres
Axis tracking: Fixed (no rotation)
Tilt: 34.0 deg
Orientation: 180.0 deg
Rated power: -
Panel material: Smooth glass with AR coating
Vary reflectivity with sun position? Yes
Correlate slope error with surface type? Yes
Slope error: 8.43 mrad

Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.816243	-117.247897	1534.07	52.00	1586.08
2	33.816242	-117.246780	1534.07	52.00	1586.08
3	33.819046	-117.246789	1534.07	52.00	1586.08
4	33.819048	-117.247899	1534.07	52.00	1586.08



2-Mile Flight Path Receptor(s)

Name: Rwy 12 Final

Description:

Threshold height : 50 ft

Direction: 135.0 deg

Glide slope: 3.0 deg

Pilot view restricted? Yes

Vertical view restriction: 30.0 deg

Azimuthal view restriction: 50.0 deg



Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.889854	-117.260195	1519.17	50.00	1569.18
2-mile point	33.910298	-117.284852	1519.17	603.46	2122.63

Name: Rwy 14

Description:

Threshold height : 50 ft

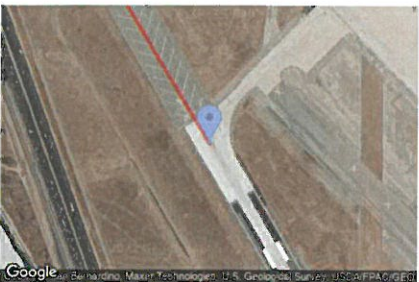
Direction: 149.0 deg

Glide slope: 3.0 deg

Pilot view restricted? Yes

Vertical view restriction: 30.0 deg

Azimuthal view restriction: 50.0 deg



Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.895961	-117.270345	1535.57	50.00	1585.58
2-mile point	33.920744	-117.288306	1535.57	603.46	2139.03

Name: Rwy 30 Final

Description:

Threshold height : 50 ft

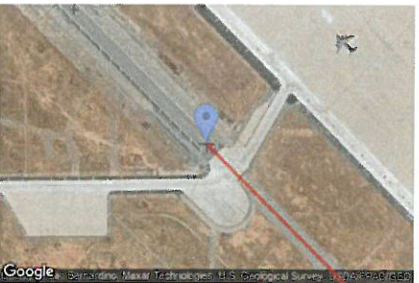
Direction: 315.0 deg

Glide slope: 3.0 deg

Pilot view restricted? Yes

Vertical view restriction: 30.0 deg

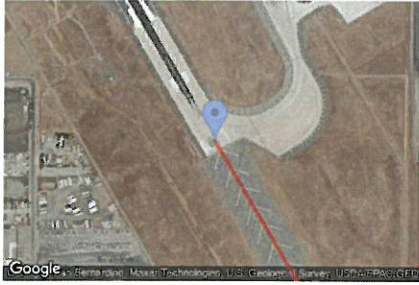
Azimuthal view restriction: 50.0 deg



Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.884521	-117.253778	1507.17	50.00	1557.18
2-mile point	33.864063	-117.229139	1507.17	603.46	2110.63

Name: Rwy 32 Final
Description:
Threshold height : 50 ft
Direction: 329.0 deg
Glide slope: 3.0 deg
Pilot view restricted? Yes
Vertical view restriction: 30.0 deg
Azimuthal view restriction: 50.0 deg

Point	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
Threshold	33.865170	-117.248411	1488.37	50.00	1538.38
2-mile point	33.840395	-117.230441	1488.37	603.46	2091.83



Route Receptor(s)

Name: Rwy 12-30 GA Rectangular Analysis

Route type Two-way

View angle: 50.0 deg

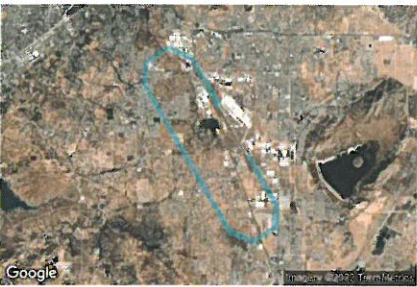


Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.884319	-117.253536	1500.07	50.00	1550.08
2	33.876069	-117.243611	1500.07	1300.06	2800.14
3	33.876081	-117.235119	1500.07	1300.06	2800.14
4	33.880814	-117.229467	1500.07	1300.06	2800.14
5	33.887897	-117.229483	1500.07	1300.06	2800.14
6	33.910333	-117.256469	1500.07	1300.06	2800.14
7	33.910322	-117.264967	1500.07	1300.06	2800.14
8	33.905592	-117.270622	1500.07	1300.06	2800.14
9	33.898508	-117.270608	1500.07	1300.06	2800.14
10	33.890258	-117.260681	1500.07	50.00	1550.08

Name: Rwy 14-32 C-17 - KC-135 Rectangular Analysis

Route type Two-way

View angle: 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.864994	-117.248281	1500.07	50.00	1550.08
2	33.836269	-117.227869	1500.07	1300.06	2800.14
3	33.821961	-117.228367	1500.07	1300.06	2800.14
4	33.813147	-117.244350	1500.07	1300.06	2800.14
5	33.819225	-117.262269	1500.07	1300.06	2800.14
6	33.908131	-117.325528	1500.07	1300.06	2800.14
7	33.922394	-117.325047	1500.07	1300.06	2800.14
8	33.931244	-117.309014	1500.07	1300.06	2800.14
9	33.925156	-117.291061	1500.07	1300.06	2800.14
10	33.896431	-117.270636	1500.07	50.00	1550.08

Name: Rwy 14-32 GA Rectangular Analysis

Route type Two-way

View angle: 50.0 deg

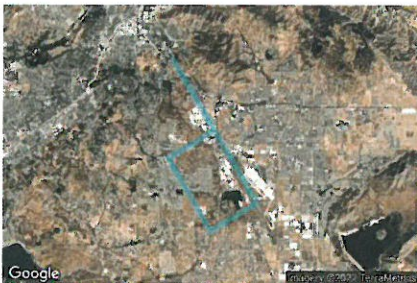


Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.864994	-117.248281	1500.07	50.00	1550.08
2	33.854942	-117.241136	1500.07	1300.06	2800.14
3	33.848078	-117.243236	1500.07	1300.06	2800.14
4	33.844669	-117.250119	1500.07	1300.06	2800.14
5	33.846422	-117.258344	1500.07	1300.06	2800.14
6	33.897972	-117.295011	1500.07	1300.06	2800.14
7	33.904833	-117.292903	1500.07	1300.06	2800.14
8	33.908242	-117.286017	1500.07	1300.06	2800.14
9	33.906486	-117.277783	1500.07	1300.06	2800.14
10	33.896431	-117.270636	1500.07	50.00	1550.08

Name: Rwy 14 Overhead Analysis

Route type One-way

View angle: 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.968036	-117.322128	1500.07	2000.10	3500.17
2	33.880706	-117.259453	1500.07	2000.10	3500.17
3	33.863564	-117.293808	1500.07	2000.10	3500.17
4	33.908131	-117.325528	1500.07	2000.10	3500.17
5	33.925156	-117.291061	1500.07	2000.10	3500.17
6	33.896431	-117.270636	1500.07	50.00	1550.08

Name: Rwy 32 Overhead
 Route type One-way
 View angle: 50.0 deg



Vertex	Latitude	Longitude	Ground elevation	Height above ground	Total elevation
	deg	deg	ft	ft	ft
1	33.793375	-117.196878	1500.07	2000.10	3500.17
2	33.880706	-117.259453	1500.07	2000.10	3500.17
3	33.863564	-117.293808	1500.07	2000.10	3500.17
4	33.819225	-117.262269	1500.07	2000.10	3500.17
5	33.836269	-117.227869	1500.07	2000.10	3500.17
6	33.864994	-117.248281	1500.07	50.00	1550.08

Discrete Observation Receptors

Number	Latitude	Longitude	Ground elevation	Height above ground	Total Elevation
	deg	deg	ft	ft	ft
1-ATCT	33.891569	-117.251175	1511.07	118.01	1629.08

1-ATCT map image



Summary of PV Glare Analysis

PV configuration and total predicted glare

PV Name	Tilt	Orientation	"Green" Glare	"Yellow" Glare	Energy Produced	Data File
	deg	deg	min	min	kWh	
Harvill Water Solar Panel	34.0	180.0	61,647	0	-	-

Distinct glare per month

Excludes overlapping glare from PV array for multiple receptors at matching time(s)

PV	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec
harvill-wate (green)	3429	8245	7679	4185	3391	2723	3139	3838	6008	9400	5214	973
harvill-wate (yellow)	0	0	0	0	0	0	0	0	0	0	0	0

PV & Receptor Analysis Results

Results for each PV array and receptor

Harvill Water Solar Panel low potential for temporary after-image

Component	Green glare (min)	Yellow glare (min)
FP: Rwy 12 Final	0	0
FP: Rwy 14	0	0
FP: Rwy 30 Final	0	0
FP: Rwy 32 Final	0	0
OP: 1-ATCT	0	0
Route: Rwy 12-30 GA Rectangular Analysis	0	0
Route: Rwy 14-32 C-17 - KC-135 Rectangular Analysis	51939	0
Route: Rwy 14-32 GA Rectangular Analysis	0	0
Route: Rwy 14 Overhead Analysis	0	0
Route: Rwy 32 Overhead	9708	0

Harvill Water Solar Panel - Receptor (Rwy 12 Final)

No glare found

Harvill Water Solar Panel - Receptor (Rwy 14)

No glare found

Harvill Water Solar Panel - Receptor (Rwy 30 Final)

No glare found

Harvill Water Solar Panel - Receptor (Rwy 32 Final)

No glare found

Harvill Water Solar Panel - OP Receptor (1-ATCT)

No glare found

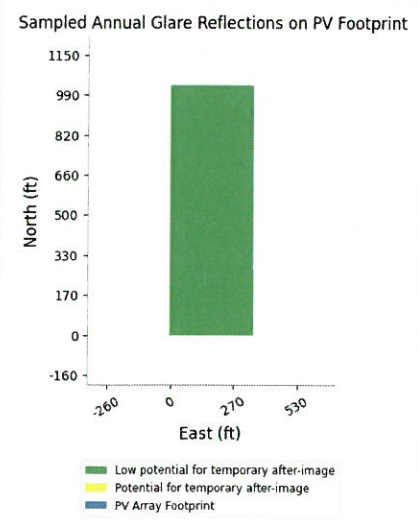
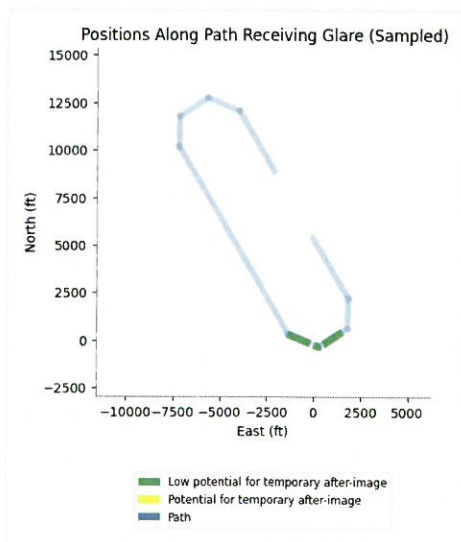
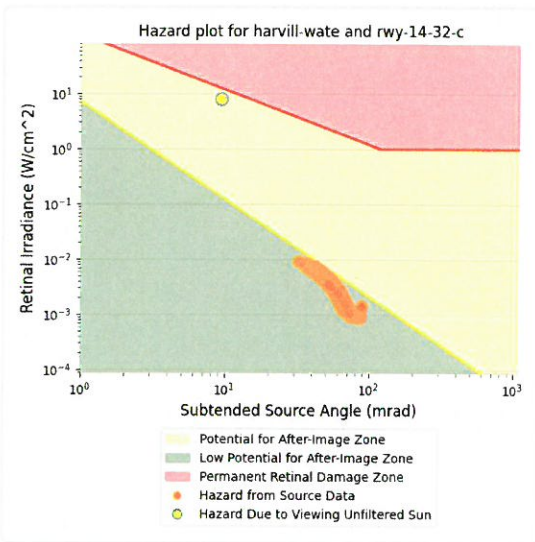
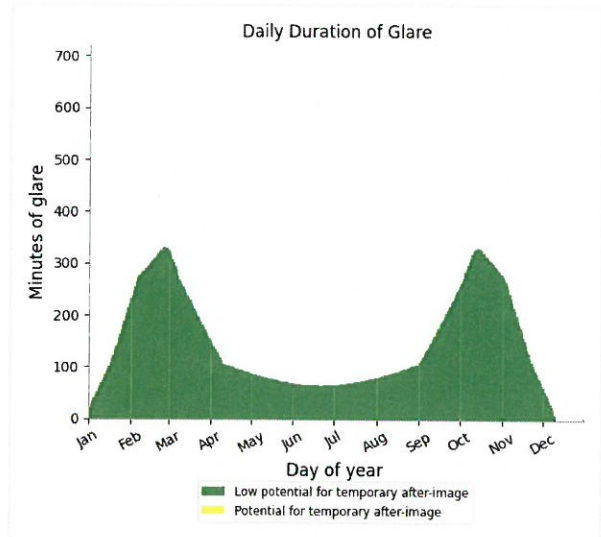
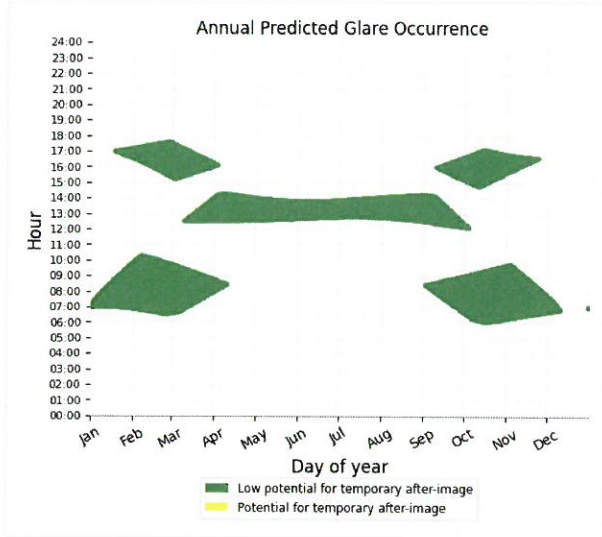
Harvill Water Solar Panel - Route Receptor (Rwy 12-30 GA Rectangular Analysis)

No glare found

Harvill Water Solar Panel - Route Receptor (Rwy 14-32 C-17 - KC-135 Rectangular Analysis)

PV array is expected to produce the following glare for receptors at this location:

- 51,939 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



Harvill Water Solar Panel - Route Receptor (Rwy 14-32 GA Rectangular Analysis)

No glare found

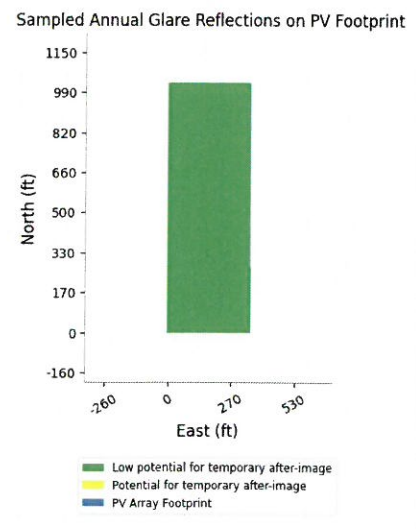
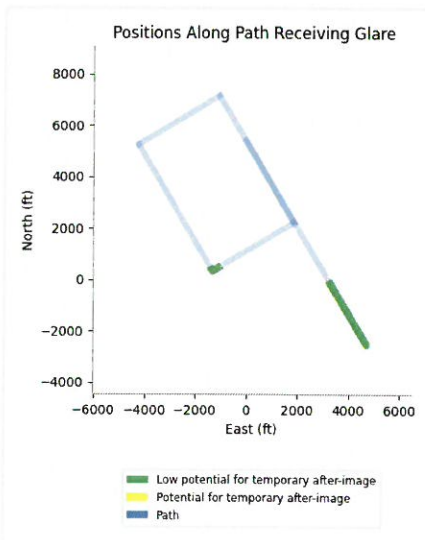
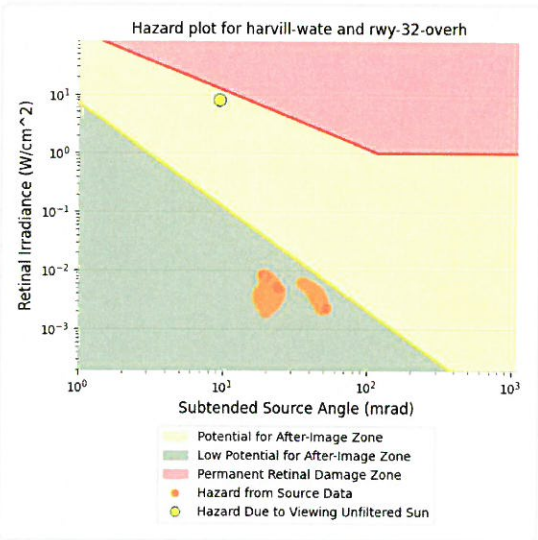
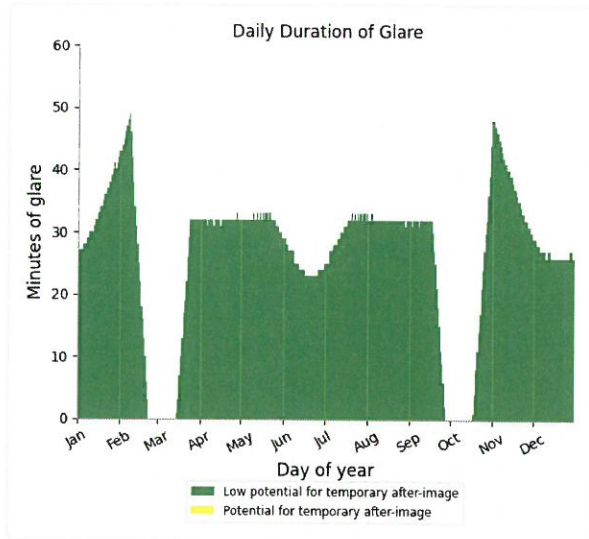
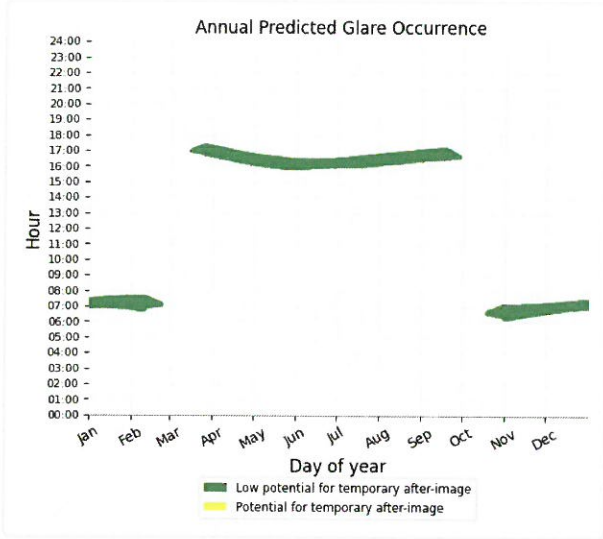
Harvill Water Solar Panel - Route Receptor (Rwy 14 Overhead Analysis)

No glare found

Harvill Water Solar Panel - Route Receptor (Rwy 32 Overhead)

PV array is expected to produce the following glare for receptors at this location:

- 9,708 minutes of "green" glare with low potential to cause temporary after-image.
- 0 minutes of "yellow" glare with potential to cause temporary after-image.



Assumptions

- Times associated with glare are denoted in Standard time. For Daylight Savings, add one hour.
- Glare analyses do not account for physical obstructions between reflectors and receptors. This includes buildings, tree cover and geographic obstructions
- Detailed system geometry is not rigorously simulated.
- The glare hazard determination relies on several approximations including observer eye characteristics, angle of view, and typical blink response time. Actual values and results may vary.
- The system output calculation is a DNI-based approximation that assumes clear, sunny skies year-round. It should not be used in place of more rigorous modeling methods.
- Several V1 calculations utilize the PV array centroid, rather than the actual glare spot location, due to algorithm limitations. This may affect results for large PV footprints. Additional analyses of array sub-sections can provide additional information on expected glare.
- The subtended source angle (glare spot size) is constrained by the PV array footprint size. Partitioning large arrays into smaller sections will reduce the maximum potential subtended angle, potentially impacting results if actual glare spots are larger than the sub-array size. Additional analyses of the combined area of adjacent sub-arrays can provide more information on potential glare hazards. (See previous point on related limitations.)
- Hazard zone boundaries shown in the Glare Hazard plot are an approximation and visual aid. Actual ocular impact outcomes encompass a continuous, not discrete, spectrum.
- Glare locations displayed on receptor plots are approximate. Actual glare-spot locations may differ.
- Glare vector plots are simplified representations of analysis data. Actual glare emanations and results may differ.
- Refer to the **Help page** for detailed assumptions and limitations not listed here.

NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. **Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org.** The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893.**

The County of Riverside Planning Department should be contacted on non-ALUC issues. For more information please contact County of Riverside Planner Deborah Bradford at (951) 922-6645.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to prull@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please contact Barbara Santos at (951) 955-5132.

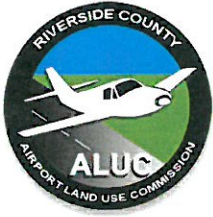
PLACE OF HEARING: Riverside County Administration Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside California

DATE OF HEARING: July 14, 2022

TIME OF HEARING: 9:30 A.M.

CASE DESCRIPTION:

ZAP1518MA22 – BCI IV Harvill Industrial Center (Representative: EPD Solutions) – County of Riverside Case No. PPT220002 (Plot Plan). A proposal to construct a 434,823 square foot manufacturing building with mezzanines on 20.57 acres located northerly of Orange Avenue, westerly of Harvill Avenue, easterly of Tobacco Road, and southerly of Water Street. The applicant also proposes to construct a rooftop solar panel system on the proposed building (Airport Compatibility Zone C2 of the March Air Reserve Base/Inland Port Airport Influence Area).



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC STAFF ONLY

ALUC Case Number: ZAP1518MA22

Date Submitted: 4/26/22

AIA: March

Zone: C2

Public Hearing Staff Review

Applicant

Applicant Full Name: BCIF Harvill Business Center LP

Applicant Address: 4675 MARARTHUR COURT, SUITE 625 NEWPORT BEACH 92660

Phone: 949-892-4904 Email: pschafer@aresmgmt.com

Representative/ Property Owner Contact Information

Representative: EPD Solutions - Michael Brown

Email: michael@epdsolutions.com

Phone: (951)639-7689

Address: 2355 Main Street, Suite 100 Irvine CA 92614

Property Owner: Same as applicant - see above.

Email: same as applicant

Phone: _____

Address: _____

Local Jurisdiction Agency

Agency Name: Riverside County

Phone: (951)955-6646

Staff Contact: Deborah Bradford

Email: dbradfor@rivco.org

Address: 4080 Lemon Street, 12th Floor Riverside CA 92501

Local Agency Case No.: PPT220002

Project Location

Street Address: West of Harvill Avenue in between Orange and Water Avenue Gross Parcel Size: 20.57

Assessor's Parcel No.: 317-270-006, -010, -015, -016

Solar

Is the project proposing solar Panels? Yes



No



If yes, please provide solar glare study.
(only if in Zone C or higher)

Data

Site Elevation:(above mean sea level) 1,538.00

Height of Building or structures: 52 feet

What type of drainage basins are being proposed and the square footage: Bio-Retention Basin | 20,983 SF

Notice

A. NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. REVIEW TIME: Estimated time for “staff level review” is approximately 30 days from date of submittal. Estimated time for “commission level review” is approximately 45 days from date of a complete application submittal to the next available commission hearing meeting.

C. SUBMISSION PACKAGE:

Please submit all application items DIGITALLY via USB or CD:

- Completed ALUC Application Form
- Plans Package: site plans, floor plans, building elevations, grading plans, subdivision maps
- Exhibits of change of zone, general plan amendment, specific plan amendment
- Project description of existing and proposed use

Additionally, please provide:

- ALUC fee payment (Checks made out to Riverside County ALUC)
- Gummed address labels of all surrounding property owners within a 300-foot radius of project site. (Only required if the project is scheduled for a public hearing).

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM: 3.5

HEARING DATE: July 14, 2022

CASE NUMBER: ZAP1525MA22 – Project Aether (Representative: Parkway Construction & Architecture)

APPROVING JURISDICTION: March Joint Powers Authority

JURISDICTION CASE NO: PP04-04A1 (Plot Plan Amendment), CUP22-01 (Conditional Use Permit)

LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

Airport Influence Area: March Air Reserve Base

Land Use Policy: Zone B2

Noise Levels: Between 65 - 75 CNEL contour from aircraft

MAJOR ISSUES: At the time the staff report was prepared, comments from the Air Force were still pending.

RECOMMENDATION: Staff recommends that the Commission CONTINUE the matter to the August 11, 2022, meeting, pending completion of the Air Force review of the project.

PROJECT DESCRIPTION: The applicant proposes to establish an aircraft manufacturing and shipping business within the former 104,000 square foot AMRO Facility as part of the existing 314,925 square foot DHL building on 30.19 acres. The applicant also proposes extending the existing building, adding a new 22,000 square foot prefabricated building for shipping and receiving, and a 17,000 square foot expandable/retractable hangar for the transport and assembly of completed winged aircraft and components.

On August 12, 2004, the Commission found consistent (under the previous 1984 Riverside County Airport Land Use Plan) ALUC case MA-04-132 for the construction of a 382,000 square foot cargo sorting facility which was occupied by DHL, and later Amazon.

PROJECT LOCATION: The project is located at 17101 Heacock Street, northerly of San Michele Road, southerly of Cardinal Avenue, and easterly of the March Air Reserve Base, approximately 1,921 feet easterly of the southerly end of Runway 14-32 at March Air Reserve Base.

BACKGROUND:

History: The following is a brief chronology of events highlighting the construction and tenant occupancy of the building:

- 2004. Approval for the DHL Air Cargo Facility. ALUC also found the project consistent.
- 2005. Building constructed and occupied by DHL.
- 2008. DHL vacates the building and remains vacant until 2011.
- 2011. AMRO fabrication and aircraft assembly business occupied a portion (100,332 square feet) of the main building. DHL portion of the building remained vacant until 2018.
- 2018. Amazon air cargo operations was approved to occupy the remaining (214,593 square feet) portion of the building.
- 2021. AMRO lease expired in 2021 and remains vacant.
- 2022. The proposed project Aether begins its planning entitlements and ALUC application to occupy the vacated AMRO area.

Non-Residential Average Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Zone B2 which limits average intensity to 100 people per acre.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan (ALUCP) and the Additional Compatibility Policies included in the March ALUCP, the following rates were used to calculate the occupancy for the proposed project:

- Manufacturing – 1 person per 200 square feet,
- Assembly – 1 person per 15 square feet,
- Warehouse – 1 person per 500 square feet, and
- Office – 1 person per 200 square feet.

The project proposes to establish an aircraft manufacturing and shipping business within the existing 104,000 square foot AMRO facility, as part of the existing 314,925 square foot DHL building. The applicant also proposes adding a new 22,000 square foot prefabricated building for shipping and receiving, and a 17,000 square foot expandable/retractable hangar for the transport and assembly of completed winged aircraft and components. This includes 93,723 square feet of manufacturing area, 40,000 square feet of warehouse area, 13,000 square feet of office area, and 1,701 square feet of assembly area, accommodating a total occupancy of 727 people.

In order to calculate the average acre intensity of the 30.19 acre parcel, the occupancy of the existing adjacent 180,000 square foot Amazon business was also needed, which accommodated 360 people. The total occupancy of the proposed Aether project (727 people) and the existing Amazon business (360 people) is 1,087 people, which results in an average intensity of 36 people per acre, which is consistent with the Zone B2 average intensity criterion of 100 people per acre.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle). Based on the number of parking spaces provided (342 standard vehicles), the total occupancy would be estimated at 513 people, resulting in an average intensity of 17 people per acre, which is consistent with the Compatibility Zone B2 average intensity criterion of 100 people per acre.

Non-Residential Single-Acre Intensity: Compatibility Zone B2 limits maximum single-acre intensity to 250 people. There are no risk-reduction design bonuses available, as March Air Reserve Base/Inland Port Airport is primarily utilized by large aircraft weighing more than 12,500 pounds.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre area would include 21,998 square feet of manufacturing area, 5,136 square feet of warehouse area, 1,000 square feet of lobby area, 600 square feet of break room area, 1,100 square feet of first floor office area, and 3,000 square feet of second floor office mezzanine area, resulting in a single acre occupancy of 248 people, which is consistent with the Compatibility Zone B2 single acre criterion of 250.

March Air Reserve Base/United States Air Force Input: Given that the project's vicinity is located near the primary runway at March Air Reserve Base, the March Air Reserve Base staff was notified of the project and sent a package of plans for their review. At the time this staff report was prepared, comments were still pending from the Air Force (although comments were received from the Base's Airfield Operations Manager, Bart Darnell, indicating that the project would have no impact on air traffic/air operations).

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zone B2.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being in an area between 65 - 75 CNEL range from aircraft noise. While the proposed industrial areas are not a noise-sensitive use and would not require special measures to mitigate aircraft-generated noise, such measures may be required to achieve reduced interior noise levels of 45 dBA CNEL in office areas as required pursuant to the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan.

Part 77: The elevation of Runway 14-32 at its southerly terminus is 1,488 feet above mean sea level (AMSL). At a distance of approximately 1,921 feet from the project to the nearest point on the runway, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,507 feet AMSL. The site's elevation is 1,520 feet AMSL and the proposed building height is 63 feet, resulting in a top point elevation of 1,583 feet AMSL. Therefore, review of the building for height/elevation reasons by the FAA Obstruction Evaluation Service (FAAOES) was required.

The applicant has submitted Form 7460-1, and the FAA OES has assigned Aeronautical Study Nos. 2022-AWP-1488-NRA thru 2022-AWP-1493-NRA to this project. A Final Determination letter was issued with no objections to the project as long as the project complies with the requirements set forth in FAA Advisory Circular 150/5370-2 "Operational Safety on Airports During construction". The FAA OES conditions have been incorporated into ALUC's conditions listed below.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not

to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33C).

Although the nearest portion of the proposed project is located within 10,000 feet of the runway (approximately 1,921 feet), the site is already developed, and the project does not propose any basins.

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight or circling climb following takeoff or toward an aircraft engaged in a straight or circling final approach toward a landing at an airport, other than a DoD or FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight or circling climb following takeoff or towards an aircraft engaged in a straight or circling final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, congregate care facilities, hotels/motels, places of assembly (including but not limited to places of worship and theaters), buildings with more than 3 aboveground habitable floors, and critical community infrastructure facilities.
 - (f) Highly noise-sensitive outdoor nonresidential uses. Examples of noise-sensitive outdoor nonresidential uses that are prohibited include, but are not limited to, major spectator-oriented sports stadiums, amphitheaters, concert halls and drive-in theaters.
 - (g) Other Hazards to flight.

3. Prior to issuance of building permits, the landowner shall convey an aviation easement to the March Inland Port Airport Authority or its successor in interest, or provide evidence that such easement has been previously conveyed. The Airport Authority may waive this requirement in the event that the Authority determines that pre-existing aviation easements dedicated to the United States of America are sufficient to address its needs. Contact the March Joint Powers Authority at (951) 656-7000 for additional information.
4. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

5. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
6. Noise attenuation measures shall be incorporated into the design of the office areas of the building, to the extent such measures are necessary to ensure that interior noise levels from aircraft operations are at or below 45 CNEL.
7. The project has been evaluated as 93,723 square feet of manufacturing area, 40,000 square feet of warehouse area, 13,000 square feet of office area, and 1,701 square feet of assembly area. Any increase in building area, change in use to any higher intensity use, change in building location, or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.
8. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar

glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission and March Air Reserve Base.

X:\AIRPORT CASE FILES\March\ZAP1525MA22\ZAP1525MA22sr.doc



Federal Aviation Administration

April 07, 2022

TO: March Inland Port Airport Authority
 Attn: Gary Gosliga
 23555 Meyer Dr.
 Riverside, CA 92518
 gosliga@marchjpa.com

CC: UNITED STATES AIR FORCE
 2685 GRAEBER STREET, BLDG 395
 MARCH ARB, CA 92518
 gosliga@marchjpa.com

CC: March Inland Port Airport Authority
 Attn: Gary Gosliga
 23555 Meyer Dr.
 Riverside, CA 92518
 gosliga@marchjpa.com

RE: (See attached Table 1 for referenced case(s))
 FINAL DETERMINATION

Table 1 - Letter Referenced Case(s)

ASN	Prior ASN	Location	Latitude (NAD83)	Longitude (NAD83)	AGL (Feet)	AMSL (Feet)
2022-AWP-1488-NRA		RIVERSIDE,CA	33-52-16.86N	117-14-49.10W	33	1520
2022-AWP-1489-NRA		RIVERSIDE,CA	33-52-17.30N	117-14-48.00W	33	1520
2022-AWP-1490-NRA		RIVERSIDE,CA	33-52-16.04N	117-14-48.56W	63	1550
2022-AWP-1491-NRA		RIVERSIDE,CA	33-52-16.50N	117-14-47.49W	63	1550
2022-AWP-1492-NRA		RIVERSIDE,CA	33-52-15.24N	117-14-48.05W	33	1520
2022-AWP-1493-NRA		RIVERSIDE,CA	33-52-15.71N	117-14-47.01W	33	1520

Description: Construct retractable hangar. Longitudes/Latitudes consider hangar in expanded configuration. Estimated used in expanded configuration 6-14 times per year. 1,520' AMSL (ground elevation 1,487 plus 33') on corners and the peak is 1,550' AMSL (ground elevation 1,487 plus 63') 1,525' east of RWY 14/32.

We do not object with conditions to the construction described in this proposal provided:

You comply with the requirements set forth in FAA Advisory Circular 150/5370-2, "Operational Safety on Airports During Construction."

The proponent is required to coordinate all associated activities with the Airport Manager/Airport Traffic Control Tower (ATCT) 5 business days prior to the beginning of the project.

This determination is subject to review if disruption to FAA Operations should occur.

The new development must be coordinated with the Airport Sponsor and Airport District Office and included on the next update to the Airport Layout Plan (ALP).

HQ AFRC/A3OA (Terminal Instrument Procedures office) performed analysis and identified no impacts to instrument procedures for March Air Reserve Base Runway 14/32.

A separate notice to the FAA is required for any construction equipment, such as temporary cranes, whose working limits would exceed the height and lateral dimensions of your proposal.

This determination does not constitute FAA approval or disapproval of the physical development involved in the proposal. It is a determination with respect to the safe and efficient use of navigable airspace by aircraft and with respect to the safety of persons and property on the ground.

In making this determination, the FAA has considered matters such as the effects the proposal would have on existing or planned traffic patterns of neighboring airports, the effects it would have on the existing airspace structure and projected programs of the FAA, the effects it would have on the safety of persons and property on the ground, and the effects that existing or proposed manmade objects (on file with the FAA), and known natural objects within the affected area would have on the airport proposal.

This determination expires on October 7, 2023 unless:

(a) extended, revised or terminated by the issuing office.

(b) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for the completion of construction, or the date the FCC denies the application.

NOTE: Request for extension of the effective period of this determination must be obtained at least 15 days prior to expiration date specified in this letter.

If you have any questions concerning this determination contact Lloyd E. Lewis (424) 405-7316 lloyd.e.lewis@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-AWP-1488-NRA.

Lloyd E. Lewis

DivUser

Signature Control No: 517075508-522613805

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

**THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS**

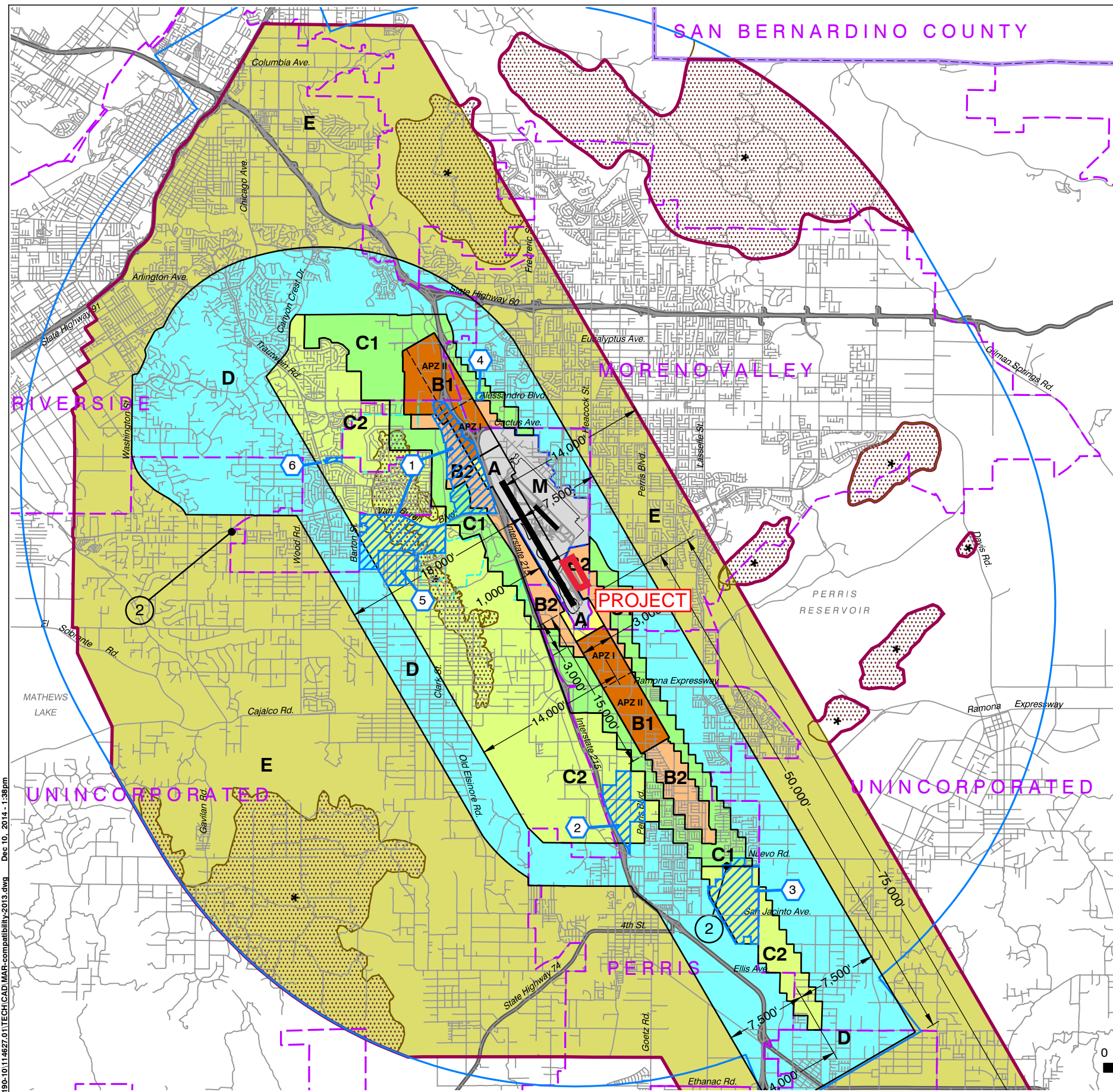
**PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES**



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



LEGEND

Compatibility Zones

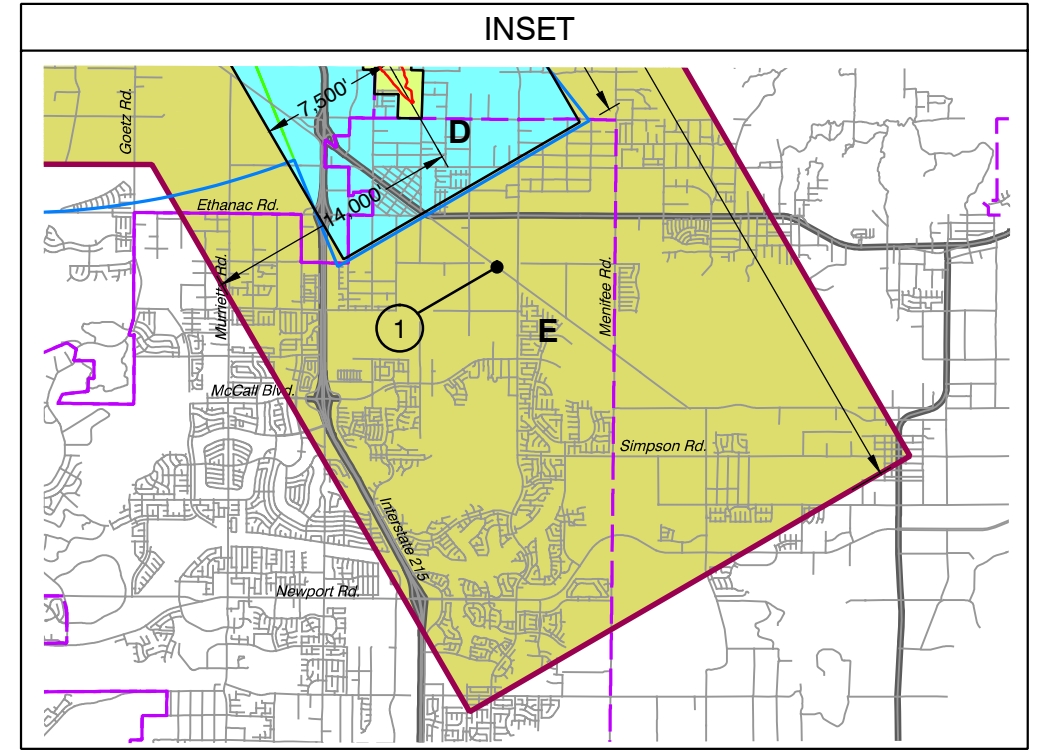
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

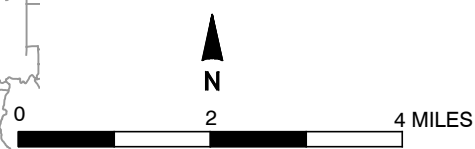
- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



**Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)**

Note:
All dimensions are measured from runway ends and centerlines.



Base map source: County of Riverside 2013

X:\18190-10\114627\01\TECH\CAD\MAR-compatibility\2013.dwg Dec 10, 2014 - 1:38pm

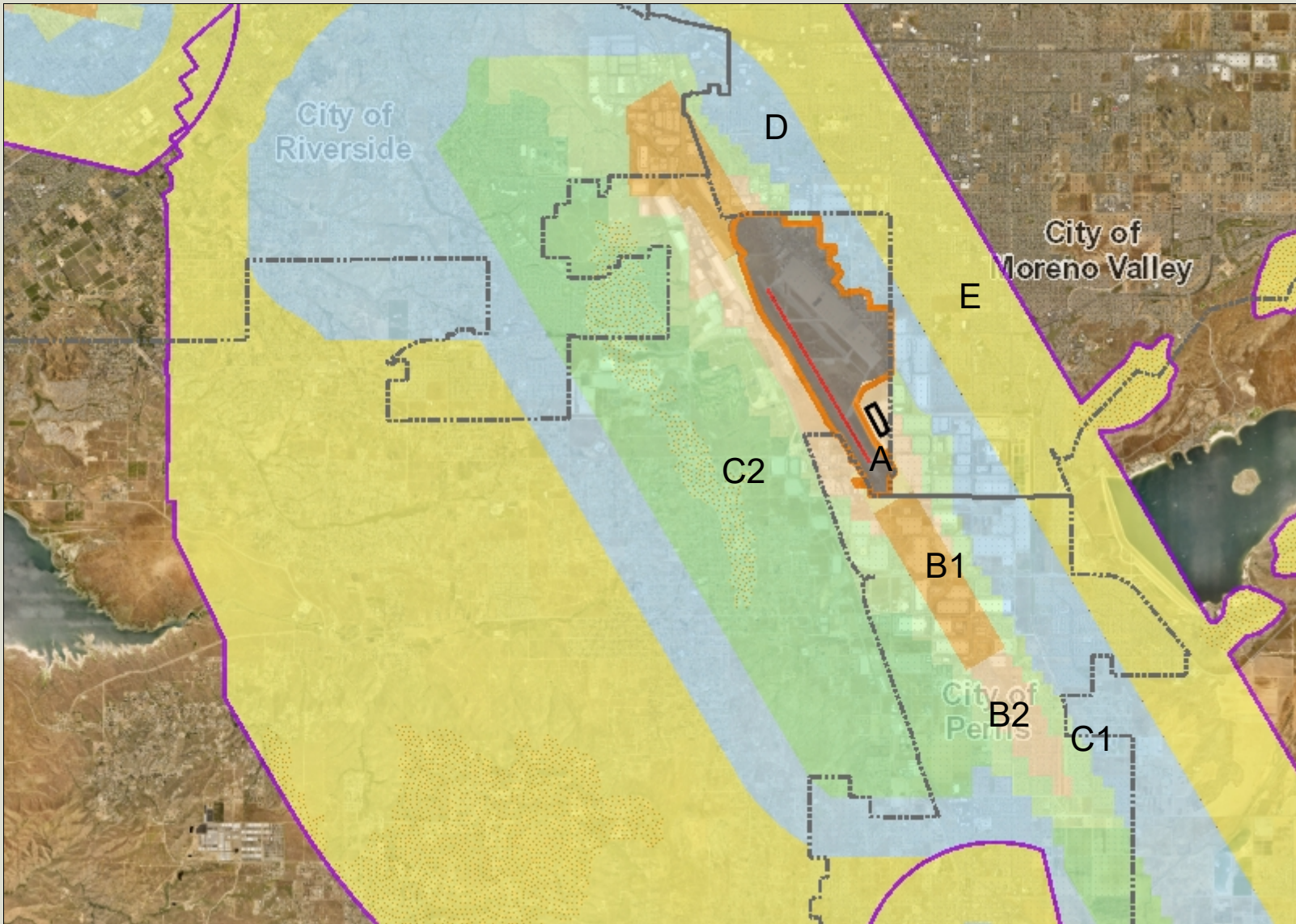
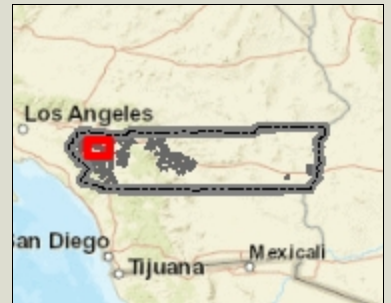
Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map MA-1

**Compatibility Map
March Air Reserve Base / Inland Port Airport**

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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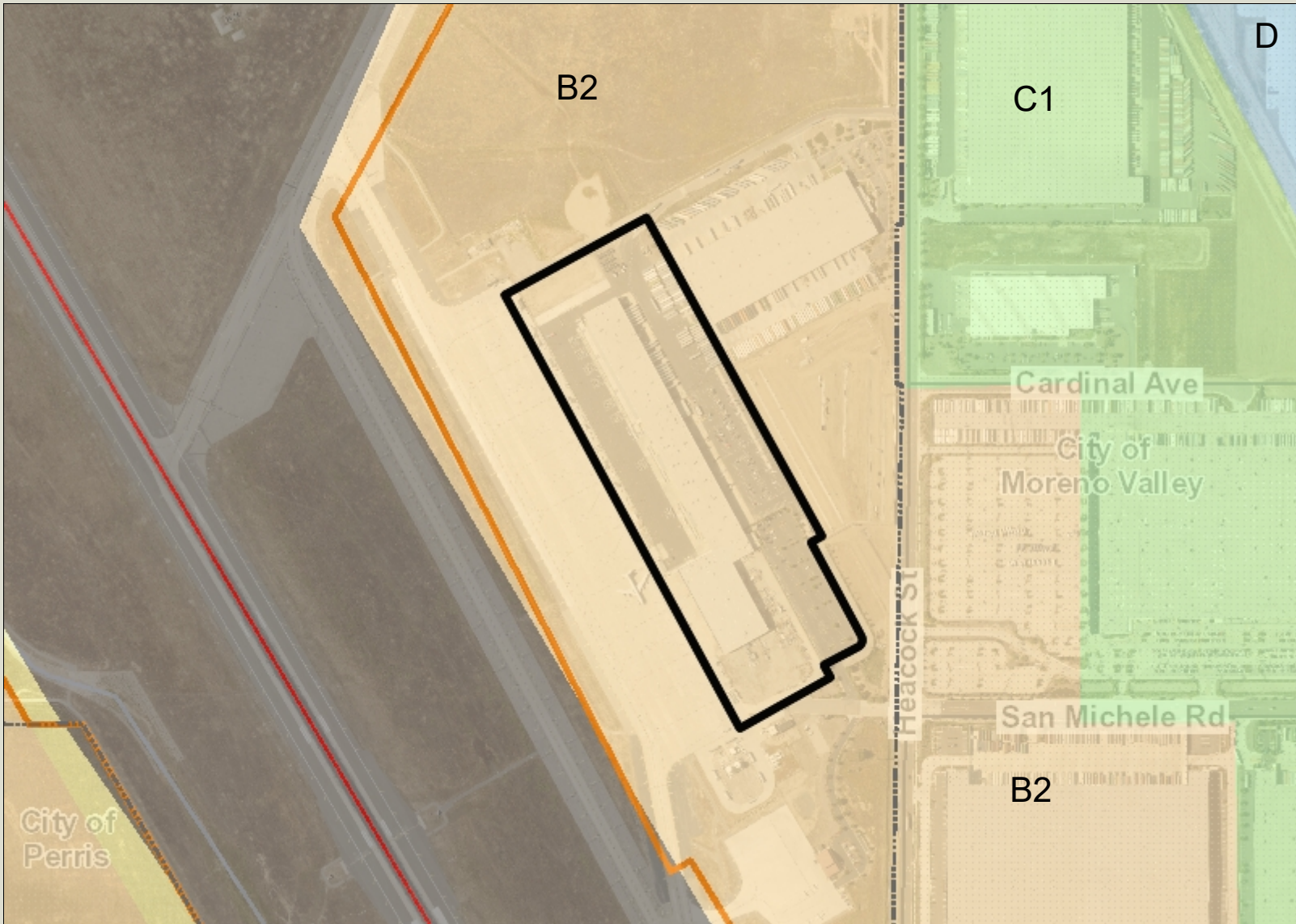


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Notes

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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Notes

Map My County Map



Legend

- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map

City of
Perris

Cardinal Ave

City of
Moreno Valley

Heacock St

San Michele Rd



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Notes

0 770 1,539 Feet



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Map My County Map



Legend

-  City Areas
-  World Street Map



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Notes

Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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Notes

Map My County Map



Legend

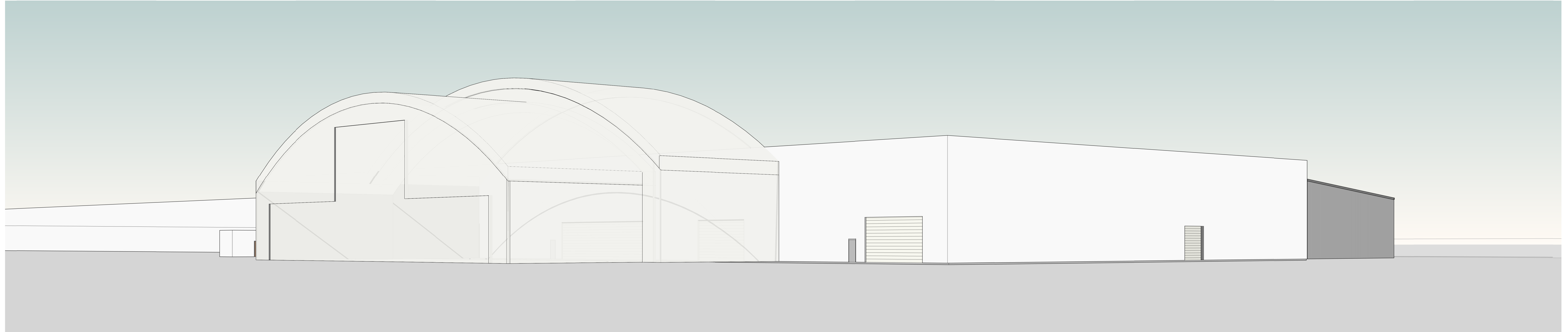
- Blueline Streams
- City Areas
- World Street Map



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Notes





Project Aether

17101 HEACOCK ST, MORENO VALLEY, CA

May 31, 2022

List of Drawings

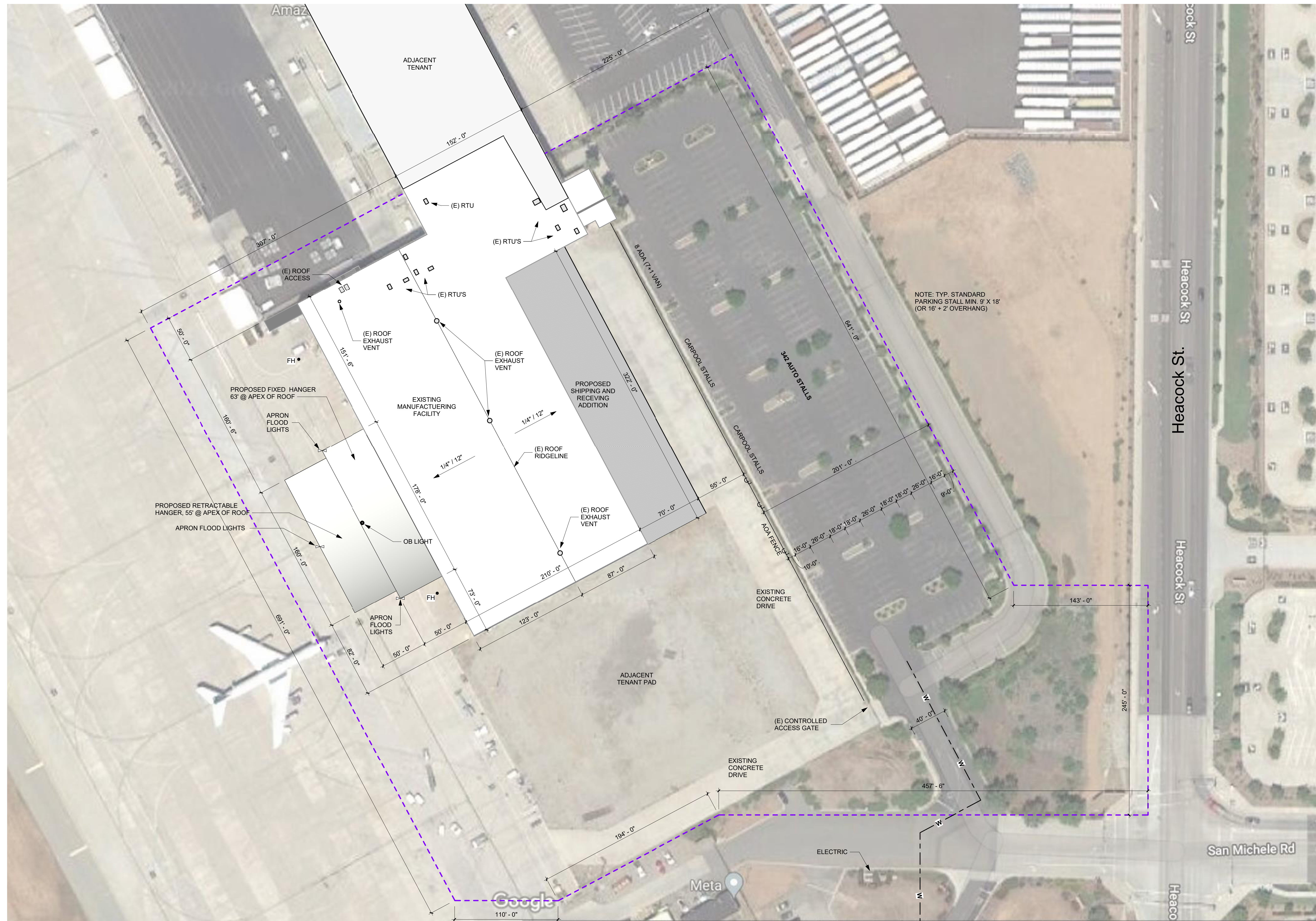
Sheet Number	Sheet Name	Date
CVR	Cover Sheet	May 31, 2022
1	Site Plan	May 31, 2022
2	Floor Plan	May 31, 2022
3	Exterior Elevations	May 31, 2022

ADDRESS: 17101 HEACOCK ST, MORENO VALLEY, CA 92551
 APN: 294180052
 EXISTING ZONE DESIGNATION: AVIATION
 TOTAL EXIST. LEASE AREA: 103,970 SF
 EXIST. OFFICE AREA: 12,985 SF
 NEW RETRACTABLE HANGAR AREA: 16,569 SF
 NEW RECEIVING AND STOR. AREA: 21,770 SF
 PROVIDED PARKING: 342 SPACES w/ 8 ADA (7+1 VAN)
 REQUIRED PARKING PER AREA
 AVIATION: 91 SPACES (1/1000)
 OFFICE: 50 SPACES (1/250)
 NEW RECEIVING & STOR.: 22 SPACES (1/1000)
 NEW RETRACTABLE HANGAR: N/A

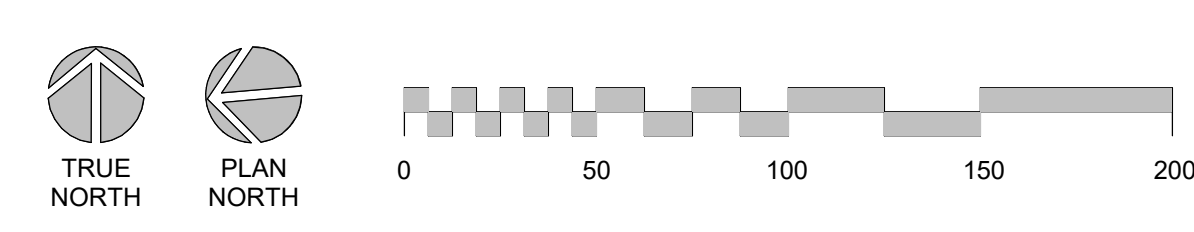
APPLICANT: PROJECT AETHER
 ADDRESS: 1700 FLIGHT WAY, SUITE 300, TUSTIN, CA 92606
 ENGINEER / LAND SURVEYOR:
 REGISTRATION NUMBER:
 ADDRESS:
 PHONE:
 UTILITY PROVIDERS
 ELECTRIC: EDISON
 GAS: SOUTHERN CALIFORNIA GAS
 TELEPHONE: VERIZON
 WATER / SEWER: WESTERN MUNICIPAL WATER DISTRICT

SITE PLAN GENERAL NOTES

- ALL NEW OR EXISTING UTILITY LINES LESS THAN 60 KV ON OR CONTIGUOUS TO THE SITE SHALL BE INSTALLED OR RELOCATED UNDER GROUND.
- ALL OUTDOOR STORAGE AREAS FOR MATERIALS AND EQUIPMENT SHALL BE FULLY SCREENED FROM VIEW.
- ELECTRICAL PANELS SHALL BE FULLY RECESSED INTO THE BUILDING ELEVATION OR SHALL BE SCREENED BY A DECORATIVE WALL OR LANDSCAPE EQUAL TO OR EXCEEDING THE HEIGHT OF THE S.E.S. PANEL. DESIGN PLANS SHALL NOTE THE TENTATIVE LOCATION OF ELECTRICAL UTILITY VAULTS. CONSTRUCTION PLANS SHALL IDENTIFY THE FINAL LOCATION OF UTILITY VAULTS.
- ROOF MOUNTED MECHANICAL EQUIPMENT SHALL BE FULLY SCREENED BY A PARAPET WALL EQUAL TO OR EXCEEDING THE HEIGHT OF THE MECHANICAL UNITS. AT A MINIMUM, BUILDINGS SHALL INCORPORATE A PARAPET WALL MEASURING 24" AROUND ALL PORTIONS OF THE BUILDING. TO THE EXTENT PERMITTED BY LAW, SATELLITE DISHES SHALL BE FULLY SCREENED BY A PARAPET WALL. GROUND MOUNTED MECHANICAL EQUIPMENT SHALL BE FULLY SCREENED FROM PUBLIC VIEW BY A COMBINATION OF DECORATIVE WALLS AND DENSE LANDSCAPING.
- ALL BACKFLOW PREVENTERS 2" OR LARGER SHALL BE SCREENED WITH LANDSCAPE LOCATED WITHIN A 6' RADIUS OF THE BACKFLOW PREVENTER. ALL BACKFLOW PREVENTERS LESS THAN 2" SHALL BE PLACED IN A WIRE MESH BASKET AND PAINTED TO MATCH THE PRIMARY BUILDING COLOR.
- SITE LIGHTING SHALL BE LOW OR HIGH PRESSURE SODIUM, MAXIMUM 750-WATT, FULL CUT-OFF FIXTURES, WITH THE MAXIMUM LIGHT FIXTURE HEIGHT OF 25' ABOVE FINISHED GRADE, AND A MAXIMUM LIGHTING LEVEL OF .5 CANDLEFOOT AT THE PROPERTY LINE.
- ALL FREESTANDING LIGHT POLES SHALL BE LOCATED WITHIN LANDSCAPED AREAS. SITE LIGHTING SHALL COMPLY WITH A MAXIMUM HEIGHT OF 25' FOR FREESTANDING OR BUILDING MOUNTED FIXTURES, AND SITE LIGHTING FIXTURES SHALL NOT EXCEED 750 WATTS AND SHALL BE FULL CUT-OFF TYPE HIGH PRESSURE SODIUM. FUTURE CONSTRUCTION PLANS SHALL INCLUDE A POINT-BY-POINT LIGHTING ANALYSIS AND CATALOG CUTS WILL BE REQUIRED. LIGHTING AT THE PERIMETER OF THE SITE IS RESTRICTED TO A MAXIMUM LIGHTING LEVEL OF 0.5 FOOT-CANDELES AT THE PROPERTY LINE.
- CONCRETE LIGHT POLE BASES SHALL BE LOCATED WITHIN LANDSCAPE AREAS OR PAINTED TO MATCH THE PRIMARY BUILDING COLOR/FINISH TO MATCH PARKING SCREENING WALLS AND SHALL NOT EXCEED 24" ABOVE FINISHED GRADE.
- FULL SCREENING OF ALL PARKING IS REQUIRED BY MOUNDING AND CONTOURING OF LANDSCAPED AREAS, BY LANDSCAPE SHRUB, BY SCREENING WALL OR BY A COMBINATION OF THESE TECHNIQUES.
- BUILDING DOWNSPOUTS SHALL BE INTERNALIZED FOR OFFICE, COMMERCIAL AND MIXED USE DEVELOPMENTS. INDUSTRIAL AND BUSINESS PARK BUILDING ELEVATIONS WHICH ARE NOT VISIBLE FROM A PUBLIC RIGHT-OF-WAY MAY INCORPORATE EXPOSED DOWNSPOUTS.
- ALL TRASH CONTAINERS SHALL BE ENCLOSED WITHIN A MASONRY SCREENING WALL WITH FULLY OPERABLE SCREENING GATES. SCREENING GATES SHALL NOT OPEN INTO VEHICULAR DRIVE AISLES. TRASH ENCLOSURES SHALL PROVIDE A LOCATION FOR THE COLLECTION OF RECYCLABLES CONSISTENT WITH WASTE MANAGEMENT REQUIREMENTS. TRASH ENCLOSURE GATES SHALL INCORPORATE A MINIMUM OF 80% OPACITY.
- WITHIN COMMERCIAL, OFFICE AND MIXED-USE DEVELOPMENTS, VEHICULAR ACCESS POINTS AND PEDESTRIAN ACCESS WAYS SHALL INCLUDE SPECIAL PAVING TREATMENT SUCH AS INTEGRAL COLORED STAMPED CONCRETE, BOMANITE, OR SIMILAR ALTERNATIVE. LOCATION AND MATERIAL SHALL BE REVIEWED AND APPROVED BY THE PLANNING DEPARTMENT STAFF PRIOR TO THE ISSUANCE OF A BUILDING PERMIT. STAMPED AND/OR COLORED ASPHALT IS NOT PERMITTED.
- ALL EXTERIOR METAL MUST BE FINISHED OR PAINTED TO MATCH THE APPROVED PROJECT COLORS.
- ALL USES SHALL OPERATE IN A MANNER WHICH IS COMPATIBLE WITH THE NEARBY MARCH AIR RESERVE BASE/MARCH INLAND PORT. THE FOLLOWING ACTIVITIES SHALL BE PROHIBITED:
 - ANY USE WHICH WOULD DIRECT A STEADY LIGHT OR FLASHING LIGHT OF RED, WHITE, GREEN, OR AMBER COLORS ASSOCIATED WITH AIRPORT OPERATIONS TOWARD AN AIRCRAFT ENGAGED IN AN INITIAL STRAIGHT CLIMB FOLLOWING TAKEOFF OR TOWARD AN AIRCRAFT ENGAGED IN A STRAIGHT FINAL APPROACH TOWARD A LANDING AT AN AIRPORT.
 - ANY USE WHICH WOULD CAUSE SUNLIGHT TO BE REFLECTED TOWARDS AN AIRCRAFT ENGAGED IN A STRAIGHT FINAL APPROACH TOWARD A LANDING AT AN AIRPORT.
 - ANY USE WHICH WOULD GENERATE SMOKE OR WATER VAPOR OR WOULD ATTRACT LARGE CONCENTRATIONS OF BIRDS, OR WHICH MAY OTHERWISE AFFECT SAFE AIR NAVIGATION WITHIN THE AREA.
 - ANY USE WHICH WOULD GENERATE ELECTRICAL INTERFERENCE THAT MAY BE DETRIMENTAL TO THE OPERATION OF AIRCRAFT AND/OR AIRCRAFT INSTRUMENTATION.
 - BUILDINGS WITHIN THE 65DBA NOISE CONTOUR WILL INCLUDE APPROPRIATE SOUND ATTENUATION.
- PRIOR TO THE ISSUANCE OF A CERTIFICATE OF OCCUPANCY, THE TENANT SHALL RECEIVE APPROVAL OF A TRAFFIC DEMAND MANAGEMENT PLAN WHICH SHALL INCLUDE THE FOLLOWING ELEMENTS: IDENTIFICATION OF THE LOCATION OF A MINIMUM OF 30' X 42' WALL AREA FOR THE POSTING OF ALTERNATIVE TRANSPORTATION MODE INFORMATION INCLUDING FINANCIAL INCENTIVES BY RESPONSIBLE AGENCIES, TRANSIT SCHEDULES AND CARPOOLING INFORMATION.
- PRIOR TO ISSUANCE OF THE PROJECT C.O.D., EACH INDUSTRIAL, BUSINESS PARK AND MIXED USE PROJECT SHALL PROVIDE A 6-SOFT SIGN IDENTIFYING THE APPROVED TRUCK ROUTE PLAN AT ALL SERVICE DRIVEWAY LOCATIONS.
- ALL DESIGN AND CONSTRUCTION PLAN SUBMITTALS SHALL INCLUDE A DIAGRAMMATIC CALCULATION IDENTIFYING THE RELATIONSHIP OF SITE IMPROVEMENTS IN COMPLIANCE WITH FAA PART 77 AIRSPACE. ALL DEVELOPMENT REQUIRES APPROVAL OF A FAA FORM 7460-1 PRIOR TO THE ISSUANCE OF BUILDING PERMITS.
- ALL CONSTRUCTION EQUIPMENT USED FOR CONSTRUCTION ACTIVITIES SHALL BE FITTED WITH EXHAUST MUFFLING AND NOISE CONTROL FILTER DEVICES TO REDUCE NOISE IMPACTS.
- PRIOR TO THE ISSUANCE OF BUILDING PERMITS, ALL DEVELOPMENT IMPACT FEES SHALL BE PAID, INCLUSIVE OF TUMF, SCHOOL FEES, AND FIRE AND PUBLIC FACILITIES FEES.
- IF ARCHAEOLOGICAL OR PALEONTOLOGICAL RESOURCES ARE ENCOUNTERED AT THE TIME OF GRADING OR PROJECT CONSTRUCTION, ALL PROJECT WORK IN THE AREA OF THE RESOURCE SHALL CEASE UNTIL THE AREA HAS BEEN SURVEYED BY A QUALIFIED ARCHAEOLOGIST OR PALEONTOLOGIST IN CONFORMANCE WITH THE CULTURAL RESOURCE MANAGEMENT PLAN.



1 Preliminary Site Plan
 1" = 50'-0"



CONSTRUCTION • ARCHITECTURE
PKKNY
 Parkway C&A, LLP
 1000 Civic Circle Lewisville, TX 75067
 parkwayconstruction.com (972) 221-1979
 Project Manager: Contact: Construction PM 214-111-1111
 Architect: Contact: Project Architect 214-111-1111

Project Aether

17101 HEACOCK ST,
 MORENO VALLEY, CA

Owner:
 Owner
 Address
 Address

Architect:
 Parkway C&A
 1000 Civic Circle
 Lewisville, TX 75057

M/E/P Engineer:
 Company A
 Address

Structural Engineer:
 Company A
 Address

Architect: Greg Klimko

SD No.	Schematic Design	Date
1		

Drawn by: MB
 Checked by: MH, CR
 Project Number: 04-22109
 COPYRIGHT PARKWAY C&A, LP © 2022

Site Plan

NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. **Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org.** The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893.**

The March Joint Powers Authority Planning Department should be contacted on non-ALUC issues. For more information, please contact March Joint Powers Authority Planner Mr. Jeff Smith at 951-656-7000.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to prull@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please contact Barbara Santos at (951) 955-5132.

PLACE OF HEARING: **Riverside County Administration Center**
 4080 Lemon Street, 1st Floor Board Chambers
 Riverside California

DATE OF HEARING: **July 14, 2022**

TIME OF HEARING: **9:30 A.M.**

CASE DESCRIPTION:

ZAP1525MA22 – Project Aether (Representative: Parkway Construction & Architecture) – March Joint Powers Authority Case Nos. PP04-04A1 (Plot Plan Amendment), CUP22-01 (Conditional Use Permit). The applicant proposes to establish an aircraft manufacturing and shipping business within the former 104,000 square foot AMRO Facility as part of the existing 314,925 square foot DHL building on 30.19 acres located at 17101 Heacock Street, northerly of San Michele Road, southerly of Cardinal Avenue, and easterly of the March Air Reserve Base. The applicant proposes extending the existing building, adding a new 22,000 square foot prefabricated building for shipping and receiving, and a 17,000 square foot expandable/retractable hangar for the transport and assembly of completed winged aircraft and components (Airport Compatibility Zone B2 of the March Air Reserve Base/Inland Port Airport Influence Area).



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC STAFF ONLY

ALUC Case Number: ZAP1525MA22

Date Submitted: 5/31/22

AIA: March

Zone: B2

Public Hearing Staff Review

Applicant

Applicant Full Name: Project Aether

Applicant Address: Attn: Ken Michaelson | 1700 Flight Way | Tustin, CA | 92782

Phone: (323) 481-2529

Email:

Representative/ Property Owner Contact Information

Representative: Matthew Hodeaux, AIA, Senior Architect

Email: mhodeaux@pkwycon.com

Parkway Construction & Architecture

Phone: (469) 757-2233

Address: 1000 Civic Circle | Lewisville, TX | 75067

Property Owner: Alameda BC, LLC | Crow Holdings International

Email: jherrill@chindustrial.com

Attn: Jack Herrill

Phone: (626) 221-4861

Address: 527 W. 7th Street, Suite 200 | Los Angeles, CA | 90014

Local Jurisdiction Agency

Agency Name: March Joint Powers Authority

Phone: (951) 656-7000

Staff Contact: Jeffrey Smith, Principal Planner

Email: smith@marchjpa.com

Address: 14205 Meridian Parkway, Suite 140 | Riverside, CA | 92518

Local Agency Case No.: PP 04-04, Amendment 1, CUP 22-01

Project Location

Street Address: 17101 Heacock Street | Riverside, CA Gross Parcel Size: 31.94

Assessor's Parcel No.: 294 180 052

Solar

Is the project proposing solar Panels? Yes

No

If yes, please provide solar glare study.
(only if in Zone C or higher)

Data

Site Elevation:(above
mean sea level) _____

Height of Building or
structures: Existing Bldg.: 38' | Ship/Rec. Bldg.: 34' | Expandable Hangar: 55' to 63'

What type of drainage basins are
being proposed and the square
footage: No basins proposed

Notice

A. NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. REVIEW TIME: Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of a complete application submittal to the next available commission hearing meeting.

C. SUBMISSION PACKAGE:

Please submit all application items DIGITALLY via USB or CD:

- Completed ALUC Application Form
- Plans Package: site plans, floor plans, building elevations, grading plans, subdivision maps
- Exhibits of change of zone, general plan amendment, specific plan amendment
- Project description of existing and proposed use

Additionally, please provide:

- ALUC fee payment (Checks made out to Riverside County ALUC)
- Gummed address labels of all surrounding property owners within a 300-foot radius of project site. (Only required if the project is scheduled for a public hearing).

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM: 3.6

HEARING DATE: July 14, 2022

CASE NUMBER: ZAP1524MA22 – Seefried Industrial Properties
(Representative: Dan Bick)

APPROVING JURISDICTION: March Joint Powers Authority

JURISDICTION CASE NO: PP22-01 (Plot Plan)

LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use
Compatibility Plan

Airport Influence Area: March Air Reserve Base

Land Use Policy: Zone B1

Noise Levels: Between 60 - 65 CNEL contour from aircraft

MAJOR ISSUES: At the time the staff report was prepared, comments from the Air Force were still pending.

RECOMMENDATION: Staff recommends that the Commission CONTINUE the matter to the August 11, 2022, meeting, pending completion of the Air Force review of the project.

PROJECT DESCRIPTION: A proposal to construct an 80,000 square foot industrial warehouse building on 4.55 acres.

PROJECT LOCATION: The site is located northerly of Determination Drive, westerly of Meridian Parkway, and southerly of Alessandro Boulevard, approximately 8,980 feet northwesterly of the northerly end of Runway 14-32 at March Air Reserve Base.

BACKGROUND:

Non-Residential Average Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zone B1, where Zone B1 limits average intensity to 50 people per acre.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan and the Additional Compatibility Policies included in the March ALUCP, the following rates were used to calculate the occupancy for the proposed project:

- Warehouse – 1 person per 500 square feet, and

- Office – 1 person per 200 square feet.

The project proposes to construct an 80,00 square foot industrial building, which includes 77,000 square feet of warehouse area, and 3,000 square feet of office area, accommodating a total occupancy of 169 people, resulting in an average intensity of 37 people per acre, which is consistent with Zone B1 average intensity criterion of 50 people per acre.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle and 1.0 persons per trailer truck space). Based on the number of parking spaces provided (100 standard vehicles, 15 trailer spaces), the total occupancy would be estimated at 117 people for an average intensity of 26 people per acre, which is consistent with the Compatibility Zone B1 average intensity criterion of 50 people per acre.

Non-Residential Single-Acre Intensity: Compatibility Zone B1 limits maximum single-acre intensity to 100 people. There are no risk-reduction design bonuses available, as March Air Reserve Base/Inland Port Airport is primarily utilized by large aircraft weighing more than 12,500 pounds.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre area would include 40,560 square feet of warehouse area, and 3,000 square feet of office area, resulting in a single acre occupancy of 96 people which is consistent with the Compatibility Zone B1 single acre criterion of 100.

March Air Reserve Base/United States Air Force Input: Given that the project's vicinity is located near the primary runway at March Air Reserve Base, the March Air Reserve Base staff was notified of the project and sent a package of plans for their review. At the time this staff report was prepared, comments were still pending from the Air Force.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zone B1.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being in an area between 60 - 65 CNEL range from aircraft noise. While the proposed industrial areas are not a noise-sensitive use and would not require special measures to mitigate aircraft-generated noise, such measures may be required to achieve reduced interior noise levels of 45 dBA CNEL in office areas as required pursuant to the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan.

Part 77: The elevation of Runway 14-32 at its northerly terminus is 1,535 feet above mean sea level (AMSL). At a distance of approximately 8,980 feet from the project to the nearest point on the runway, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,625 feet AMSL. The site's finished floor elevation is 1,553 feet AMSL and proposed building height is 38 feet, resulting in a top point elevation of 1,591 feet AMSL. Therefore, review of the buildings for height/elevation reasons by the FAA Obstruction Evaluation Service (FAAOES) was not required.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33C).

Although the nearest portion of the proposed project is located within 10,000 feet of the runway (approximately 8,980 feet), the project utilizes underground detention systems which will not contain surface water or attract wildlife and, therefore, would not constitute a hazard to flight.

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use or activity which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than a DoD or FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport to the extent as to result in a potential for temporary after-image greater than the low ("green") level.
 - (c) Any use or activity which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, congregate care facilities, hotels/motels, places of assembly (including, but not limited to places of worship and theaters), buildings with more than 2 aboveground habitable floors, hazardous materials and critical community infrastructure facilities.

- (f) Highly noise-sensitive outdoor non-residential uses. Examples of noise-sensitive outdoor nonresidential uses that are prohibited include, but are not limited to, major spectator-oriented sports stadiums, amphitheaters, concert halls, and drive-in theaters.
 - (g) Any other uses not permitted in Accident Potential Zones I and II pursuant to DoDI 4165.57 Appendix 2, Table 1.
 - (h) Other hazards to flight.
3. Prior to issuance of any building permits, the landowner shall convey and have recorded an avigation easement to the March Inland Port Airport Authority or its successor in interested, or provide evidence that such easement has previously conveyed. The Airport Authority may waive this requirement in the event that the Authority determines that pre-existing avigation easements dedicated to the United States of America are sufficient to address its needs. Contact the March Joint Powers Authority at (951) 656-7000 for additional information.
4. The project has been conditioned to utilized underground detention systems, which shall not contain surface water or attract wildlife. Any other proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the stormwater basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the stormwater basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.
- Landscaping in the stormwater basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.
- A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin
5. Zoned fire sprinkler systems shall be required throughout the building.
6. Noise attenuation measures shall be incorporated into the design of the office areas of the structure, to the extent such measures are necessary to ensure that interior noise levels from aircraft operations are at or below 45 CNEL.

7. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
8. The project has been evaluated for an 80,000 square foot industrial warehouse building including 77,000 square feet of warehouse area, and 3,000 square feet of office area. Any change in use to any higher intensity use, change in building location, or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.
9. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission and March Air Reserve Base.

X:\AIRPORT CASE FILES\March\ZAP1524MA22\ZAP1524MA22sr.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

**THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS**

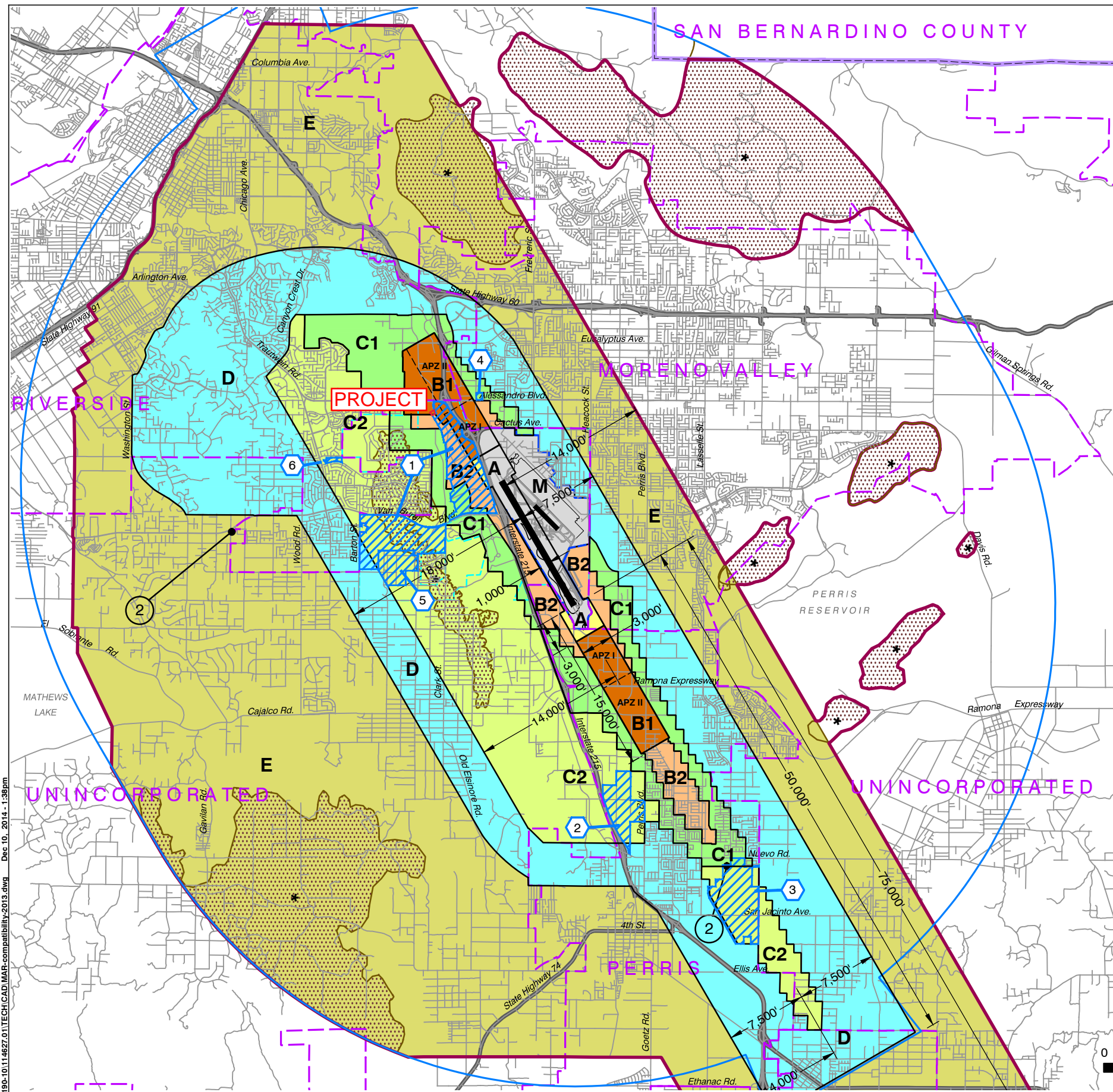
**PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES**



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



LEGEND

Compatibility Zones

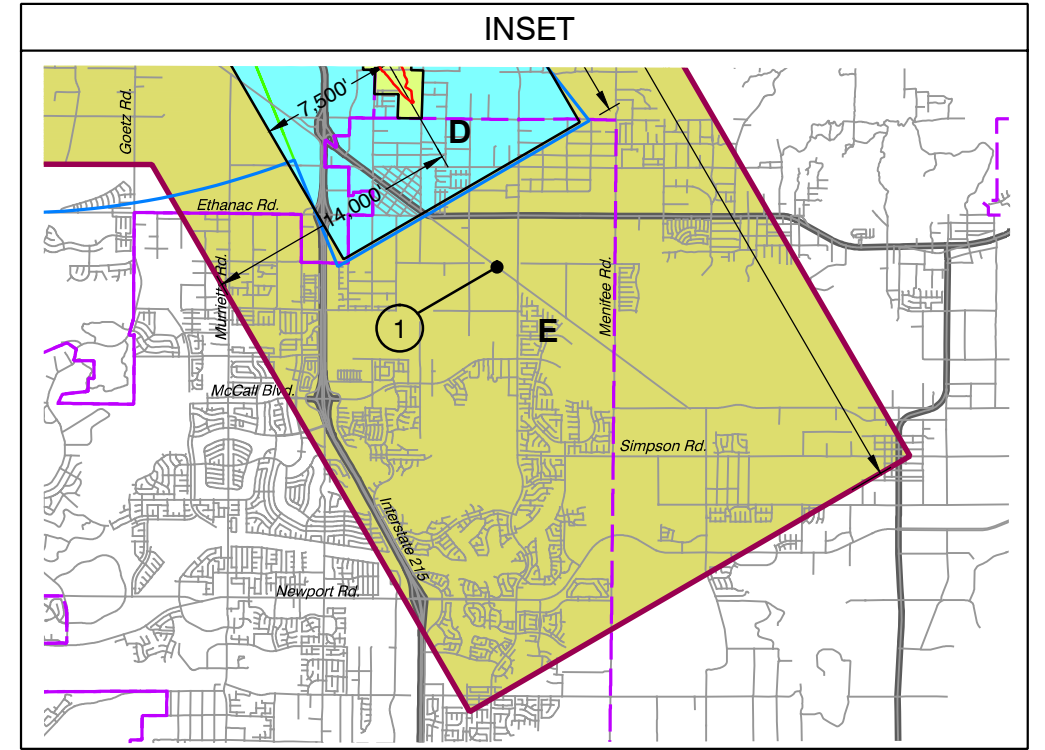
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

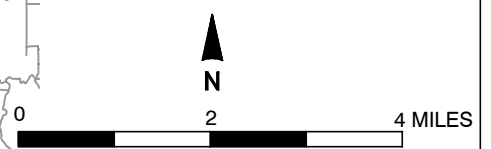
- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



**Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)**

Note:
All dimensions are measured from runway ends and centerlines.



Base map source: County of Riverside 2013

X:\18190-10\114627\01\TECH\CAD\MAR-compatibility\2013.dwg Dec 10, 2014 - 1:38pm

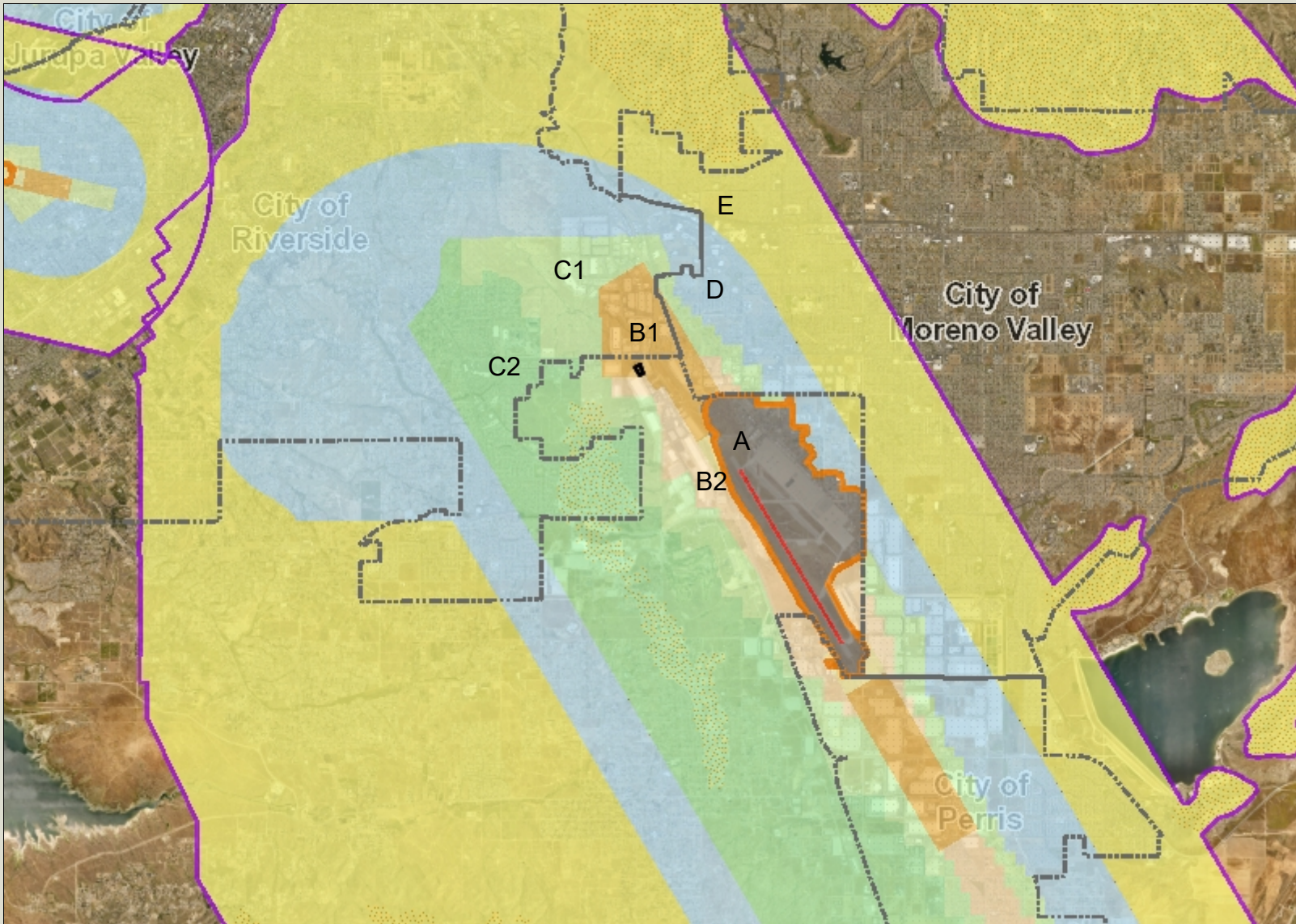
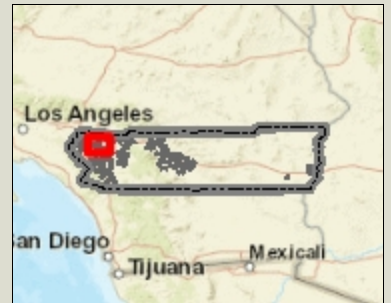
Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map MA-1

**Compatibility Map
March Air Reserve Base / Inland Port Airport**

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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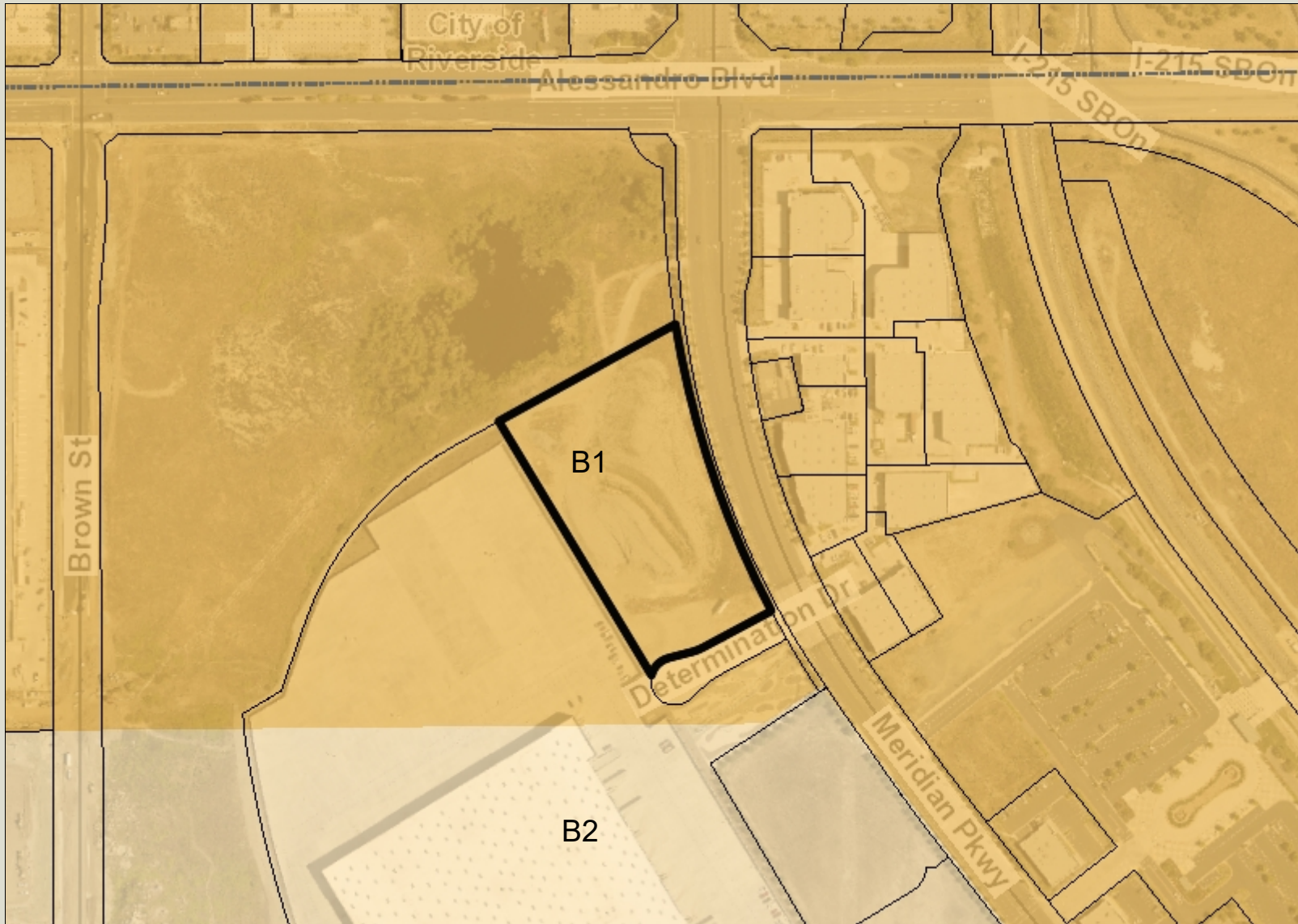


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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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0 385 770 Feet

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Notes

Map My County Map



Legend

- Parcels
- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



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Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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Notes



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Map My County Map



Legend

-  Blueline Streams
-  City Areas
-  World Street Map



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Notes



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Map My County Map



Legend

- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



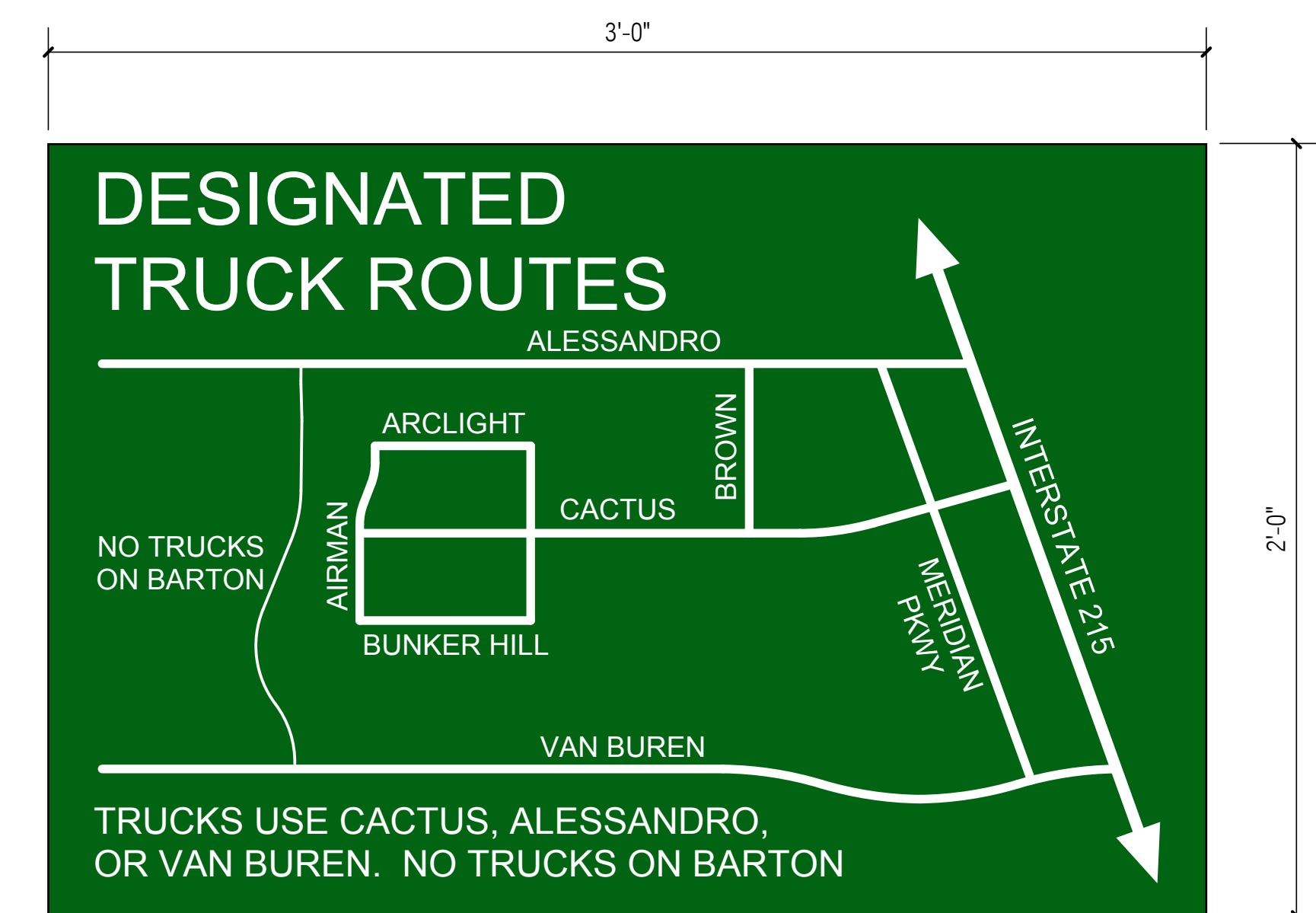
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

Notes



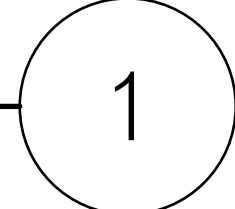
STANDARD JPA NOTES

- ALL NEW OR EXISTING UTILITY LINES LESS THAN 60 KV ON OR CONTIGUOUS TO THE SITE SHALL BE INSTALLED OR RELOCATED UNDERGROUND.
- ALL OUTDOOR STORAGE AREAS FOR MATERIALS AND EQUIPMENT SHALL BE FULLY SCREENED FROM VIEW.
- ROOF-MOUNTED MECHANICAL EQUIPMENT SHALL BE FULLY SCREENED BY A PARAPET WALL TO THE EXTENT PERMITTED BY LAW. SATELLITE DISHES SHALL BE FULLY SCREENED BY A PARAPET WALL. GROUND MOUNTED MECHANICAL EQUIPMENT SHALL BE FULLY SCREENED FROM PUBLIC VIEW BY A COMBINATION OF DECORATIVE WALLS AND DENSE LANDSCAPING.
- ALL BACKFLOW PREVENTERS 2" OR LARGER SHALL BE SCREENED WITH LANDSCAPE LOCATED WITHIN A 6' RADIUS OF THE BACKFLOW PREVENTER. ALL BACKFLOW PREVENTERS LESS THAN 2" SHALL BE PLACED IN A WIRE MESH BASKET AND PAINTED TO MATCH THE PRIMARY BUILDING COLOR.
- SITE LIGHTING SHALL BE 2700 KELVIN, MAXIMUM 750-WATT, FULL CUT-OFF FIXTURES, WITH THE MAXIMUM LIGHT FIXTURE HEIGHT OF 25' ABOVE FINISHED GRADE, AND A MAXIMUM LIGHTING LEVEL OF .5 CANDLEFOOT AT THE PROPERTY LINE.
- FULL SCREENING OF ALL PARKING IS REQUIRED BY MOUNDING AND CONTOURING OF LANDSCAPED AREAS, BY LANDSCAPE SHRUBS, BY SCREENING WALL, OR BY COMBINATION OF THESE TECHNIQUES.
- BUILDING DOWNSPOUTS SHALL BE INTERNALIZED FOR OFFICE, COMMERCIAL AND MIXED USE DEVELOPMENTS. INDUSTRIAL AND BUSINESS PARK BUILDING ELEVATIONS WHICH ARE NOT VISIBLE FROM A PUBLIC RIGHT-OF-WAY MAY INCORPORATE EXPOSED DOWNSPOUTS.
- ALL TRASH CONTAINERS SHALL BE ENCLOSED WITHIN A MASONRY SCREENING WALL WITH FULLY OPAQUE SCREENING GATES. SCREENING GATES SHALL NOT OPEN INTO VEHICULAR DRIVE AISLES. TRASH ENCLOSURES SHALL PROVIDE A LOCATION FOR THE COLLECTION OF RECYCLABLES CONSISTENT WITH WASTE MANAGEMENT REQUIREMENTS. TRASH ENCLOSURE GATES SHALL INCORPORATE A MINIMUM OF 80% OPAQIITY.
- WITHIN COMMERCIAL, OFFICE AND MIXED-USE DEVELOPMENTS (NOT APPLICABLE TO THIS PROJECT), VEHICULAR ACCESS POINTS AND PEDESTRIAN ACCESS WAYS SHALL INCLUDE SPECIAL PAVING TREATMENT SUCH AS INTEGRAL COLORED STAMPED CONCRETE, BORDERS, OR SIMILAR ALTERNATIVE. LOCATION AND MATERIAL SHALL BE REVIEWED AND APPROVED BY THE PLANNING DEPARTMENT STAFF PRIOR TO THE ISSUANCE OF A BUILDING PERMIT. STAMPED AND/OR COLORED ASPHALT IS NOT PERMITTED.
- ALL EXTERIOR METAL MUST BE FINISHED OR PAINTED TO MATCH THE APPROVED PROJECT COLORS WITH THE EXCEPTION OF ANODIZED ALUMINUM WINDOW MULLIONS.
- ALL USES SHALL OPERATE IN A MANNER WHICH IS COMPATIBLE WITH THE NEARBY MARCH AIR RESERVE BASE/MARCH INLAND PORT. THE FOLLOWING ACTIVITIES SHALL BE PROHIBITED:
 - A. ANY USE WHICH WOULD DIRECT A STEADY LIGHT OR FLASHING LIGHT OF RED, WHITE, GREEN, OR AMBER COLORS ASSOCIATED WITH AIRPORT OPERATIONS TOWARD AN AIRCRAFT ENGAGED IN AN INITIAL STRAIGHT CLIMB FOLLOWING TAKEOFF OR TOWARD AN AIRCRAFT ENGAGED IN A STRAIGHT FINAL APPROACH TOWARD A LANDING AT AN AIRPORT, OTHER THAN AN FAA-APPROVED NAVIGATIONAL SIGNAL LIGHT OR VISUAL APPROACH SLOPE INDICATOR.
 - B. ANY USE WHICH WOULD CAUSE SUNLIGHT TO BE REFLECTED TOWARDS AN AIRCRAFT ENGAGED IN AN INITIAL STRAIGHT CLIMB FOLLOWING TAKEOFF OR TOWARD AN AIRCRAFT ENGAGED IN A STRAIGHT FINAL APPROACH TOWARD A LANDING AT AN AIRPORT.
 - C. ANY USE WHICH WOULD GENERATE SMOKE OR WATER VAPOR OR WOULD ATTRACT LARGE CONCENTRATIONS OF BIRDS, OR WHICH MAY OTHERWISE AFFECT SAFE AIR NAVIGATION WITHIN THE AREA.
 - D. ANY USE WHICH WOULD GENERATE ELECTRICAL INTERFERENCE THAT MAY BE DETRIMENTAL TO THE OPERATION OF AIRCRAFT AND/OR AIRCRAFT INSTRUMENTATION.
- BUILDINGS WITHIN THE 65DBA NOISE CONTOUR WILL INCLUDE APPROPRIATE SOUND ATTENUATION.
- PRIOR TO THE ISSUANCE OF A CERTIFICATE OF OCCUPANCY, THE TENANT SHALL RECEIVE APPROVAL OF A TRAFFIC DEMAND MANAGEMENT PLAN WHICH SHALL INCLUDE THE FOLLOWING ELEMENTS: IDENTIFICATION OF THE LOCATION OF A MINIMUM OF 30' X 42' WALL AREA FOR THE POSTING OF ALTERNATIVE TRANSPORTATION MODE INFORMATION INCLUDING FINANCIAL INCENTIVES BY RESPONSIBLE AGENCIES, TRANSIT SCHEDULES AND CARPOOLING INFORMATION.
- PRIOR TO ISSUANCE OF THE PROJECT C OF D, EACH PROJECT SHALL PROVIDE A 6-SQ/FT SIGN IDENTIFYING THE APPROVED TRUCK ROUTE PLAN AT ALL SERVICE DRIVEWAY LOCATIONS.
- PRIOR TO ISSUANCE OF THE PROJECT C OF D, EACH PROJECT SHALL PROVIDE A 6-SQ/FT SIGN IDENTIFYING THE 5-MINUTE TRUCK IDLING MAXIMUM, WITH A MINIMUM OF ONE SIGN FOR EVERY 2 ROLL-UP TRUCK DOORS.
- ALL DESIGN AND CONSTRUCTION PLAN SUBMITTALS SHALL INCLUDE A DIAGRAMMATIC CALCULATION IDENTIFYING THE RELATIONSHIP OF SITE IMPROVEMENTS IN COMPLIANCE WITH FAA PART 77 AIRSPACE. ALL DEVELOPMENT REQUIRES APPROVAL OF A FAA FORM 7460-1 PRIOR TO THE ISSUANCE OF BUILDING PERMITS.
- ALL CONSTRUCTION EQUIPMENT USED FOR CONSTRUCTION ACTIVITIES SHALL BE FITTED WITH EXHAUST MUFFLERS AND NOISE CONTROL FILTER DEVICES TO REDUCE NOISE IMPACTS.
- PRIOR TO THE ISSUANCE OF BUILDING PERMITS, ALL DEVELOPMENT IMPACT FEES SHALL BE PAID, INCLUSIVE OF TUMF, SCHOOL FEES, AND FIRE AND PUBLIC FACILITIES FEES.
- IF ARCHAEOLOGICAL OR PALEONTOLOGICAL RESOURCES ARE ENCOUNTERED AT THE TIME OF GRADING OR PROJECT CONSTRUCTION, ALL PROJECT WORK IN THE AREA OF THE RESOURCE SHALL CEASE UNTIL THE AREA HAS BEEN SURVEYED BY A QUALIFIED ARCHAEOLOGIST OR PALEONTOLOGIST IN CONFORMANCE WITH THE CULTURAL RESOURCE MANAGEMENT PLAN.

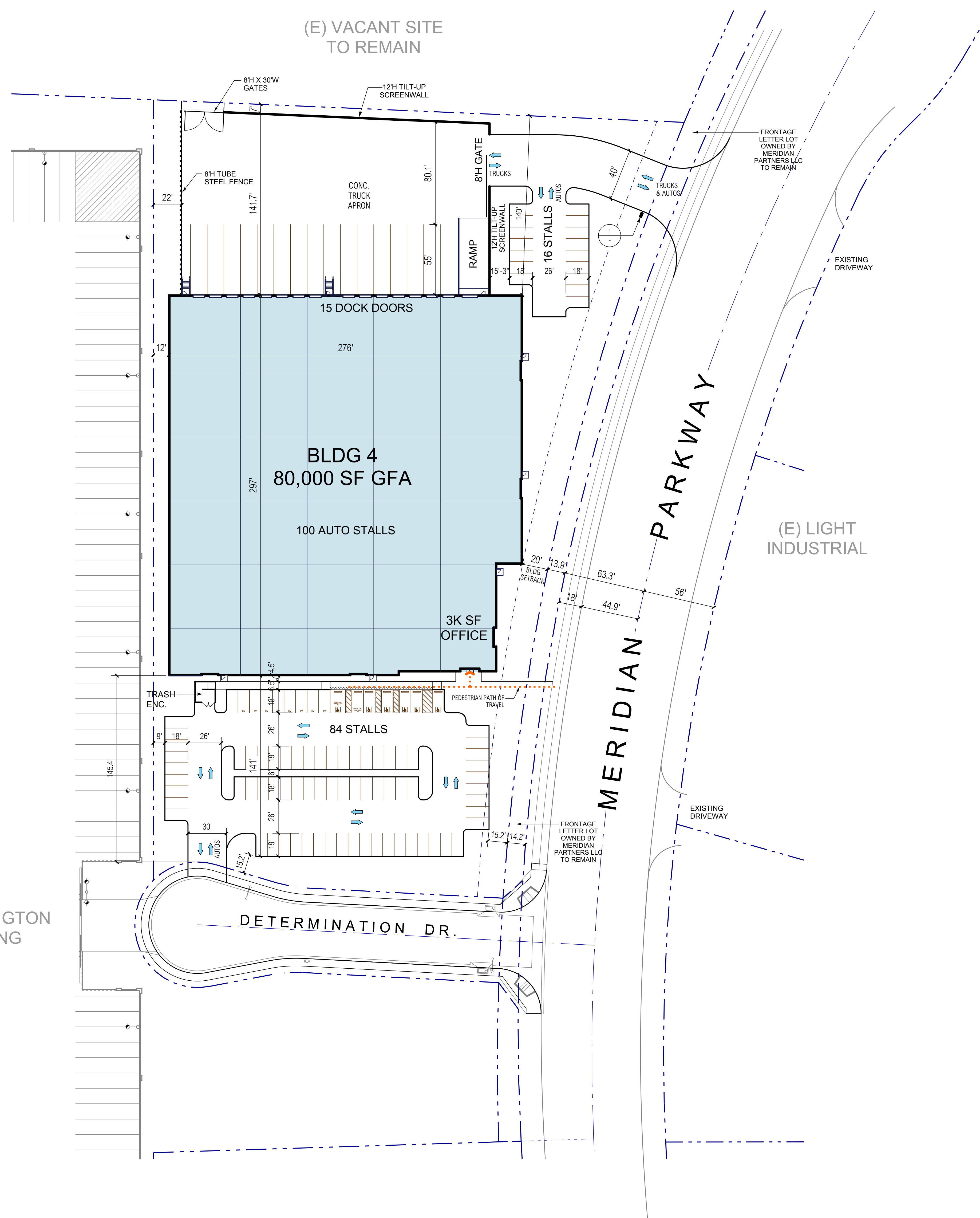


2'-0" X 3'-0" X 0.080" ALUMINUM PANEL LAMINATED GREEN HP VINYL WITH WHITE HP VINYL COPY AND GRAPHICS. PANELS TO THEN BE COATED WITH ANTI GRAFFITI SHEETING. PANEL TO COME WITH MINIMUM A 2/3/8" GALVANIZED POST AND MOUNTING HARDWARE. PROVIDE SHOP DRAWING TO ARCHITECT AND MJPA FOR APPROVAL PRIOR TO FABRICATION.

TRUCK ROUTE SIGN



(E) VACANT SITE TO REMAIN



PROJECT DATA

SITE AREA: 198,238 SF
4.55 AC (1 AC MIN.)

SETBACKS:
FRONT (MERIDIAN PKWY) 20' (20' MIN.)
REAR 12' (3' MIN.)
NORTH SIDE 140' (3' MIN.)
SOUTH SIDE (DETERMINATION DR.) 145.4' (20' MIN.)

ALLOWABLE HEIGHT:
BASIC ALLOWABLE 35'
10% INCREASE IN MAX. ALLOWABLE 38'-6"
PROPOSED BUILDING HEIGHT 38'-6"
PROPOSED TOWER ELEMENT AT CORNER 42'-0"

BUILDING AREA:
FOOTPRINT 80,000 SF
MEZZANINE 0 SF
TOTAL BUILDING AREA: 80,000 SF

COVERAGE 40.35 %
FAR .403

PARKING REQUIRED:
3,000 SF OFFICE @ 3.3/1000 10 STALLS
0 - 50,000 SF @ 1/1000 50 STALLS
50,000 - 200,000 SF @ 0.33/1000 9 STALLS
TOTAL STALLS REQUIRED 69 STALLS

AUTO PARKING PROVIDED:
STANDARD STALLS 85 STALLS
CARPOOL / EV STALLS (10%) 10 STALLS
ACCESSIBLE STALLS 5 STALLS
TOTAL STALLS PROVIDED 100 STALLS

LOADING DOCK POSITIONS 15 DOCKS

GRADE DOOR POSITIONS 1 DOORS

TRUCK TRAILER STALLS 12'X53' 0 STALLS

BIKE PARKING:
REQUIRED @ 1/20 AUTO STALLS 6
PROVIDED 6

LANDSCAPE AREA REQUIRED @ 10%: 19,825 SF

LANDSCAPE AREA PROVIDED: 50,010 SF
25.0 %

GENERAL PROJECT INFORMATION:

ZONE: BPX - MARCH JPA DEVELOPMENT CODE

ASSESSOR PARCEL NUMBERS: 297-100-083

LEGAL DESCRIPTION: LOT 1 OF TRACT NO. 37107 AS SHOWN BY MAP ON FILE IN BOOK 463, PAGES 1 THROUGH 9, INCLUSIVE OF MAPS, RECORDS OF RIVERSIDE COUNTY, CALIFORNIA.

PLANNING CASE NUMBERS: PP22-01

CODE ANALYSIS:

CONSTRUCTION TYPE TYPE III-B

FIRE SPRINKLERS FULLY SPRINKLERED - EFSR

AREA JUSTIFICATION:
SIDEYARDS (4) 40' - 60' SIDEYARDS
ALLOWABLE AREA UNLIMITED AREA - SECTION 507.3

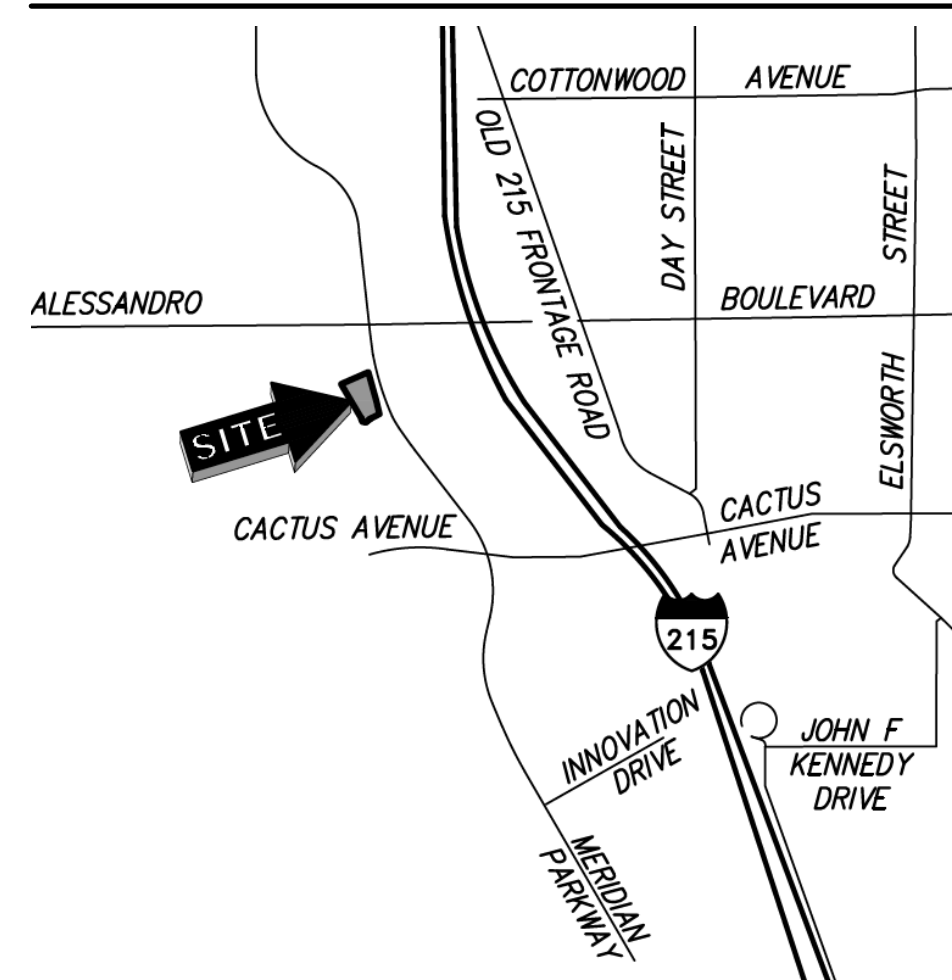
ALLOW HEIGHT 55' + 20' = 75 FEET - SECTION 504.2

PROPOSED HEIGHT 42'-0"

UTILITY PROVIDERS

ELECTRICAL: EDISON
WATER/SEWER: WESTERN MUNICIPAL WATER DISTRICT
TELEPHONE: VERIZON
GAS: SOUTHERN CALIFORNIA GAS

VICINITY MAP



SCALE: 1" = 40'-0"



RG
Office of Architectural Design
15231 Alton Parkway, Suite 100
Irvine, CA 92618
T 949-341-0920
FX 949-341-0922

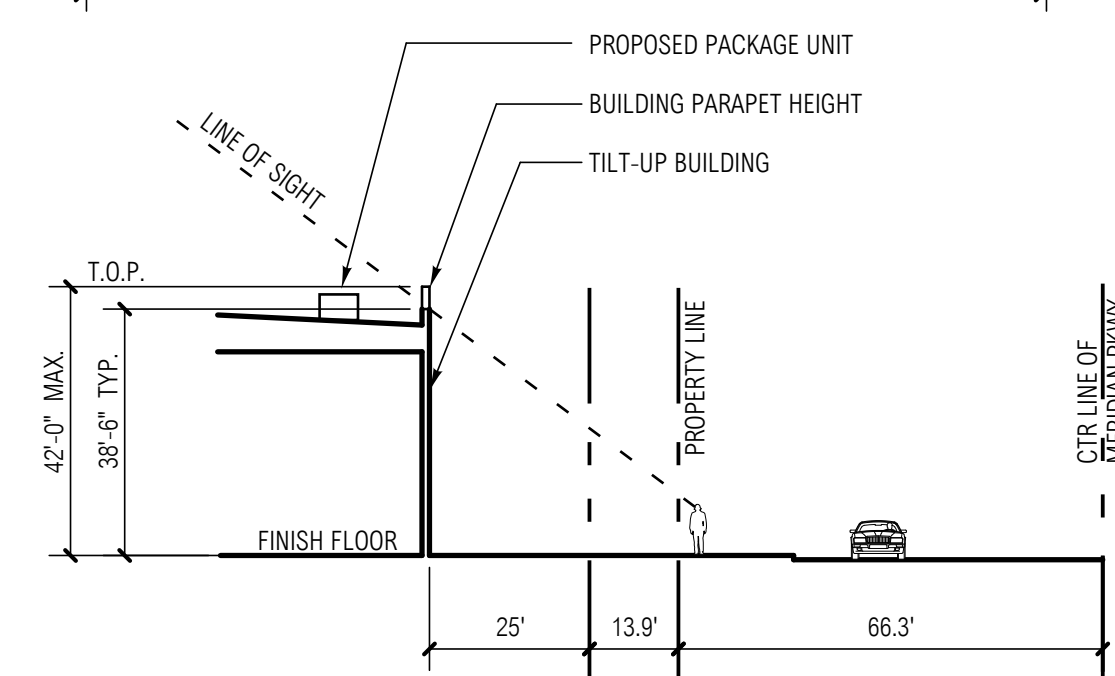
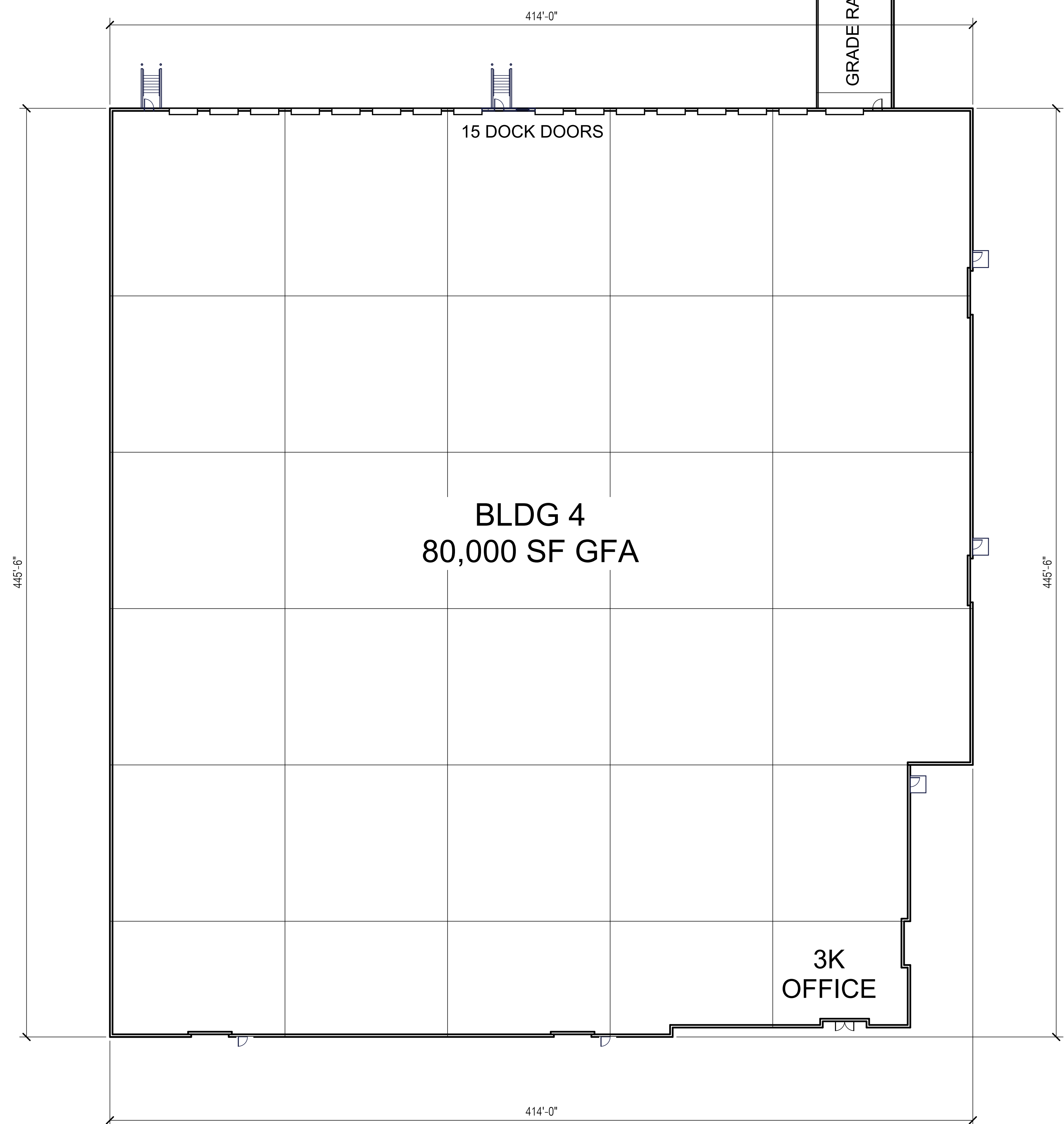
SEEFRIED
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scottirwin@seefriedproperties.com

MERIDIAN
WEST CAMPUS - BUILDING 4
MERIDIAN PARK LLC

MARK	DATE	DESCRIPTION
SD	6/16/22	PLOT PLAN PACKAGE SUBMITTAL
SD	6/13/22	PLOT PLAN PACKAGE SUBMITTAL
SD	1/25/22	PLOT PLAN PACKAGE SUBMITTAL
SD	1/11/22	PREAPPLICATION SITE PLAN
SD	12/17/21	SCHEMATIC SITE PLAN

RG PROJECT NO:	21134.00
CAD FILE NAME:	21134-04-A1-01
DRAWN BY:	CS
CHK'D BY:	CS
COPYRIGHT: RGA, OFFICE OF ARCHITECTURAL DESIGN	
SHEET TITLE	

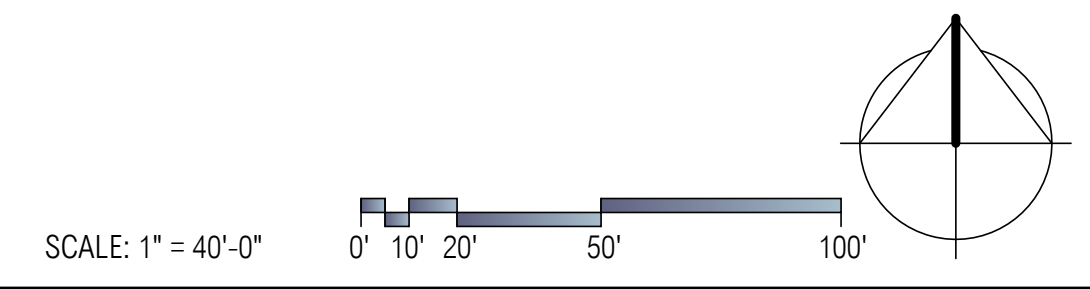
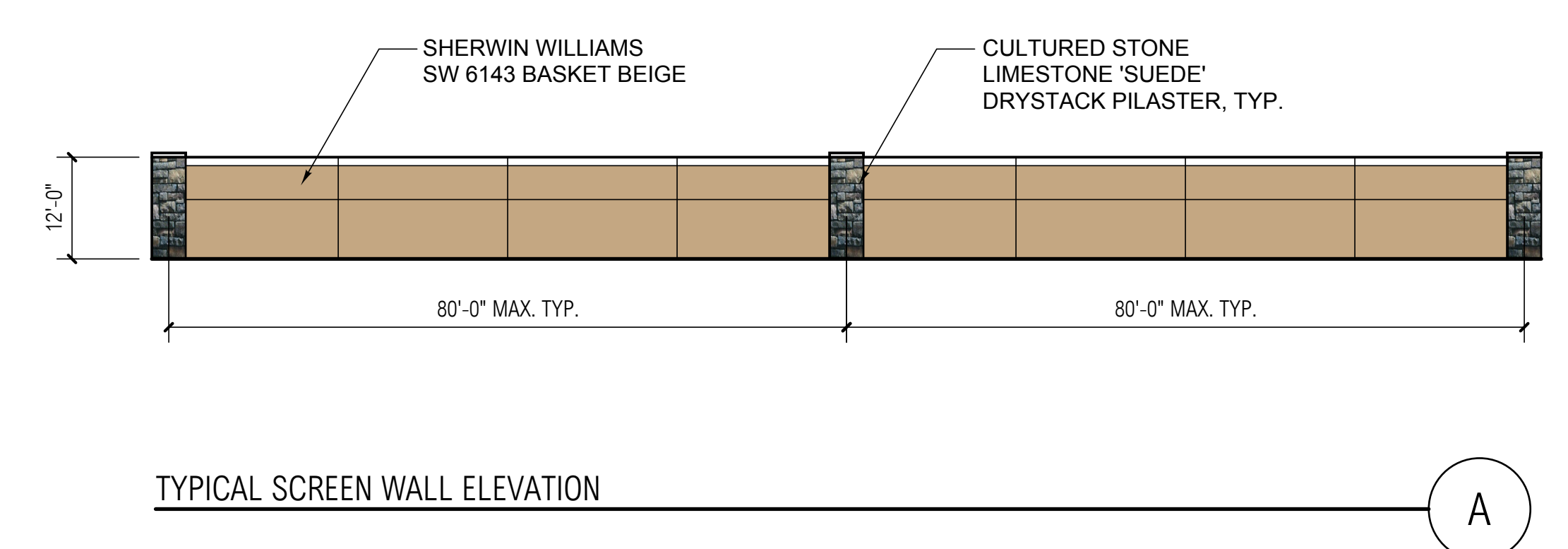
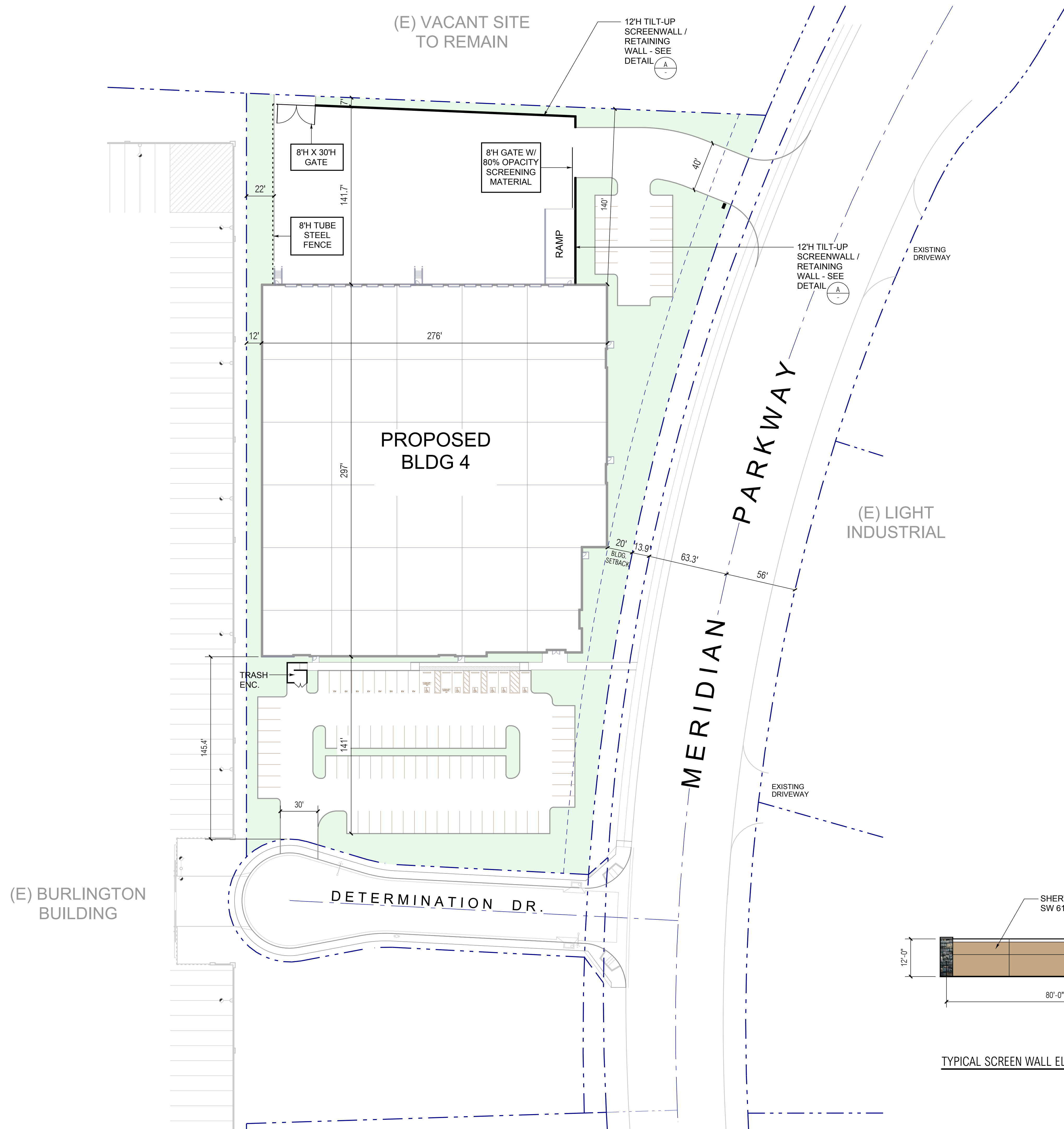
A1-01



TYPICAL EQUIPMENT SCREEN LINE OF SIGHT
 SCALE 1" = 30'-0"
 NOTE: LINE OF SIGHT TAKEN FROM 6'-0" ABOVE FINISH GRADE

MARK	DATE	DESCRIPTION
SD	6/16/22	PLOT PLAN PACKAGE SUBMITTAL
SD	4/20/22	PLOT PLAN PACKAGE SUBMITTAL
SD	1/25/22	PLOT PLAN PACKAGE SUBMITTAL

RG	PROJECT NO:	21134.00
RG	CAD FILE NAME:	21134-04-A2-01
RG	DRAWN BY:	AMB
RG	CHK'D BY:	CS
RG	COPYRIGHT:	RG, OFFICE OF ARCHITECTURAL DESIGN
SHEET TITLE		
FLOOR & ROOF PLAN		
A2-01		

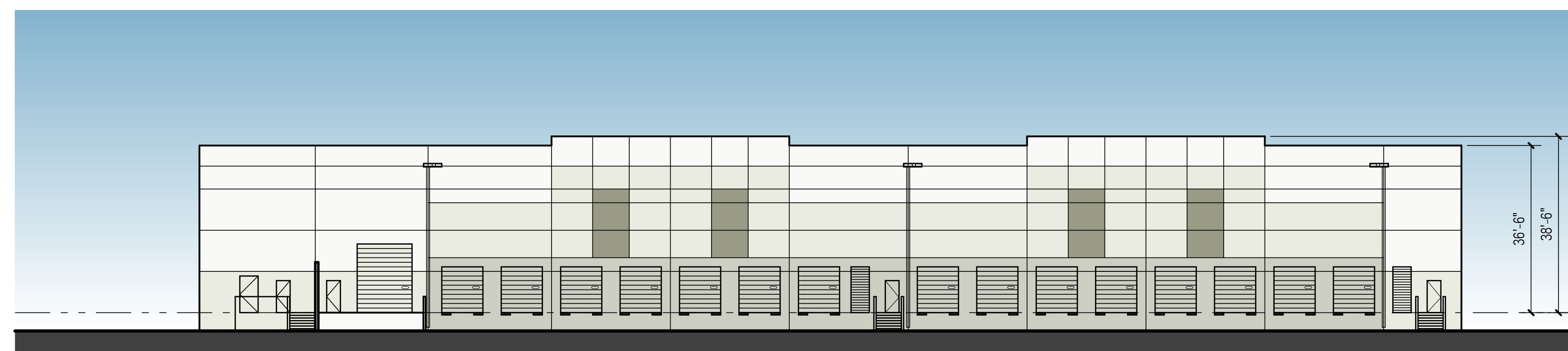


MARK	DATE	DESCRIPTION
SD	4/20/22	PLOT PLAN PACKAGE SUBMITTAL
SD	1/25/22	PLOT PLAN PACKAGE SUBMITTAL
SD	12/17/21	SCHEMATIC SITE PLAN

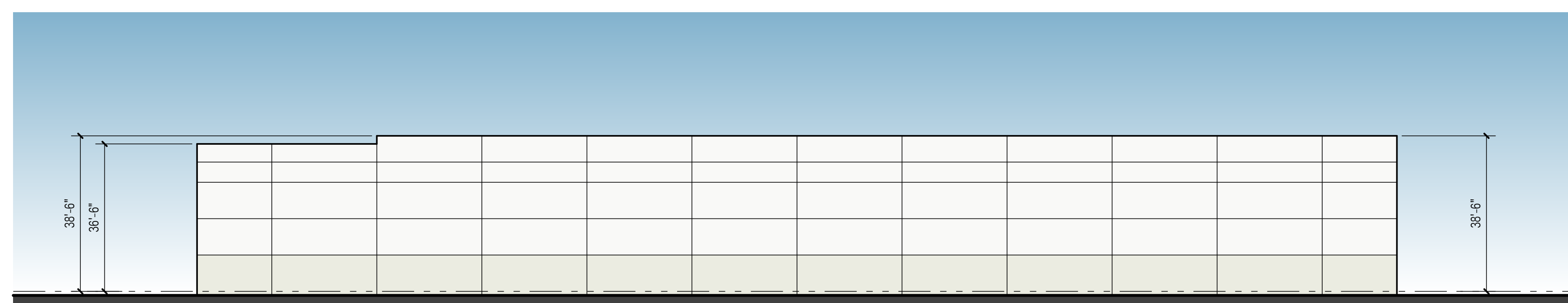
RG A PROJECT NO:	21134.00
CAD FILE NAME:	21134-04-A1-02
DRAWN BY:	CS
CHK'D BY:	CS
COPYRIGHT:	RG A, OFFICE OF ARCHITECTURAL DESIGN
SHEET TITLE:	A1-02

FINISH SCHEDULE

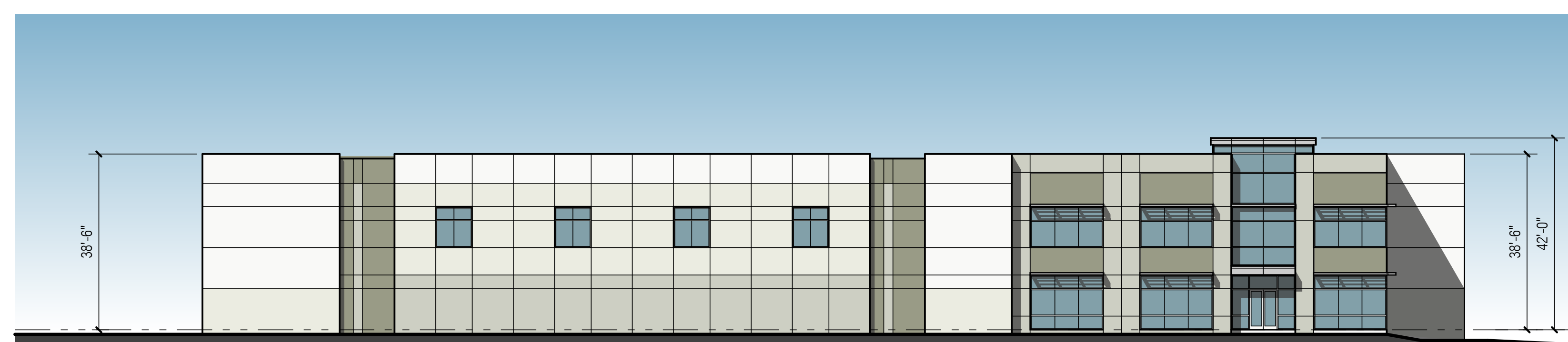
- 1. FIELD COLOR: SHERWIN WILLIAMS 7008 EXTRA WHITE
- 2. ACCENT COLOR: SHERWIN WILLIAMS SW 6169 SEDATE GRAY
- 3. ACCENT COLOR: SHERWIN WILLIAMS SW 6199 RARE GRAY
- 4. BASE COLOR: SHERWIN WILLIAMS SW 7060 ATTITUDE GRAY
- 5. GLAZING: 1/4" MONOLITHIC 1/4" PPG SOLARCOOL PACIFICA REFLECTIVE #2 IN CLEAR ANODIZED ALUMINUM STOREFRONT. THE MAXIMUM ALLOWABLE REFLECTANCE OF GLASS SHALL BE 25%.



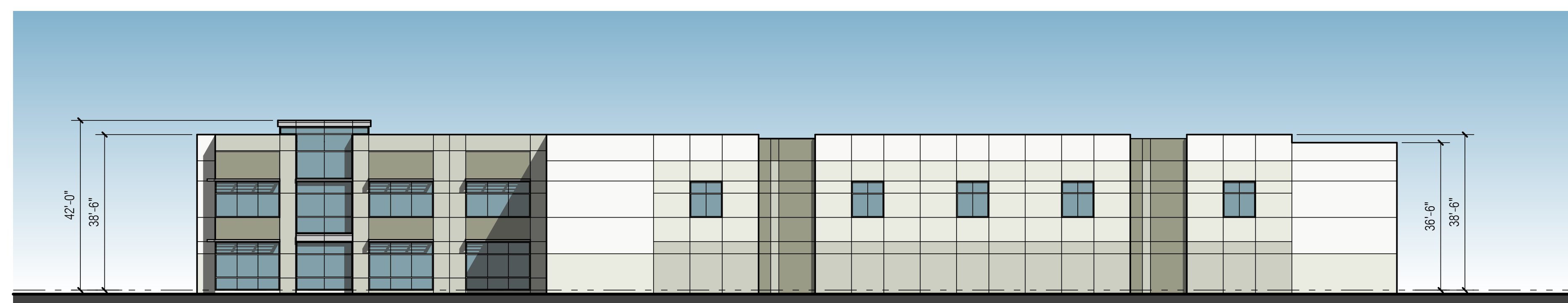
NORTH ELEVATION



WEST ELEVATION



SOUTH ELEVATION



EAST ELEVATION

SCALE: 1" = 20'-0"

SD	DATE	DESCRIPTION
SD	4/20/22	PLOT PLAN PACKAGE SUBMITTAL
SD	1/25/22	PLOT PLAN PACKAGE SUBMITTAL
MARK	DATE	DESCRIPTION

RG	PROJECT NO:	21134-00
RG	CAD FILE NAME:	21134-01-A3-01
RG	DRAWN BY:	AMB
RG	CHK'D BY:	CS
RG	COPYRIGHT:	RG, OFFICE OF ARCHITECTURAL DESIGN
RG	SHEET TITLE:	BUILDING 4 ELEVATIONS
RG		A3-01

NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. **Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org.** The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893.**

The March Joint Powers Authority Planning Department should be contacted on non-ALUC issues. For more information, please contact March Joint Powers Authority Planner Jeffrey Smith at 951-656-7000.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to prull@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please contact Barbara Santos at (951) 955-5132.

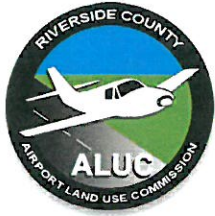
PLACE OF HEARING: **Riverside County Administration Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside California**

DATE OF HEARING: **July 14, 2022**

TIME OF HEARING: **9:30 A.M.**

CASE DESCRIPTION:

ZAP1524MA22 – Seefried Industrial Properties (Representative: Dan Bick) – March Joint Powers Authority Case No. PP22-01 (Plot Plan). A proposal to construct an 80,000 square foot industrial warehouse building on 4.55 acres, located northerly of Determination Drive, westerly of Meridian Parkway, and southerly of Alessandro Boulevard (Airport Compatibility Zone B1 of the March Air Reserve Base/Inland Port Airport Influence Area).



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC STAFF ONLY

ALUC Case Number: ZAP1524MA22

Date Submitted: 5/31/22

AIA: March

Zone: B1

Public Hearing Staff Review

Applicant

Applicant Full Name: Seefried Industrial Properties

Applicant Address: 2321 Rosecrans Ave. Suite 2220 El Segundo, CA 90245

Phone: 310-536-7900

Email: danbick@seefriedproperties.com

Representative/ Property Owner Contact Information

Representative: Dan Bick

Email: danbick@seefriedproperties.com

Phone: 310-536-7900

Address: 2321 Rosecrans Ave. Suite 2220 El Segundo, CA 90245

Property Owner: CH Realty IX/I Riverside Meridian South, L.P.

Email: danbick@seefriedproperties.com

Phone: 310-536-7900

Address: 3819 Maple Avenue Dallas, TX 75219

Local Jurisdiction Agency

Agency Name: March Joint Powers Authority

Phone: 951-656-7000

Staff Contact: Jeffrey Smith

Email: smith@marchjpa.com

Address: 14205 Meridian Parkway, Suite 140 Riverside, CA 92518

Local Agency Case No.: Plot Plan 22-01

Project Location

Street Address: NWC Meridian Parkway & Determination Dr. RIVERSIDE, CA 92518 Gross Parcel Size: 4.55 AC

Assessor's Parcel No.: 297-100-083

Solar

Is the project proposing solar Panels? Yes

No

If yes, please provide solar glare study.
(only if in Zone C or higher)

Data

Site Elevation:(above mean sea level) 1552.50

Height of Building or structures: 38'

What type of drainage basins are being proposed and the square footage: HDPE Underground Storage Basins; 12,000 SF

Notice

A. NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. REVIEW TIME: Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of a complete application submittal to the next available commission hearing meeting.

C. SUBMISSION PACKAGE:

Please submit all application items DIGITALLY via USB or CD:

- Completed ALUC Application Form
- Plans Package: site plans, floor plans, building elevations, grading plans, subdivision maps
- Exhibits of change of zone, general plan amendment, specific plan amendment
- Project description of existing and proposed use

Additionally, please provide:

- ALUC fee payment (Checks made out to Riverside County ALUC)
- Gummed address labels of all surrounding property owners within a 300-foot radius of project site. (Only required if the project is scheduled for a public hearing).

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

STAFF REPORT

ADMINISTRATIVE ITEMS

5.1 Director's Approvals

- A. During the period of May 16, 2022, through June 15, 2022, as authorized pursuant to Section 1.5.2(d) of the 2004 Riverside County Airport Land Use Compatibility Plan, ALUC Director Paul Rull reviewed three non-legislative case within the Perris Valley Airport, Palm Springs International Airport, and Hemet-Ryan Airport Influence Areas.

ZAP1025PV22 (Zone D Perris, Zone E March) pertains to City of Perris Case No. CUP21-05080 (Conditional Use Permit), a proposal to construct a modular trailer rental yard including a 6,115 square foot industrial building on 5.97 acres, located northerly of Mapes Road, westerly of Goetz Road, easterly of A street, and southerly of Artlo Avenue. The site is located within Compatibility Zone D of the Perris Valley Airport Influence Area, where non-residential intensity is restricted to 100 people per average acre and 300 people per single acre. The project proposes a 6,115 square foot building, which consist of 2,500 square feet of warehouse area, 360 square feet of office area, 135 square feet of storage area, and 1,400 square feet of assembly area, accommodating an occupancy of 101 people, resulting in an average intensity of 21 people per acre, and a single acre intensity of 101 people, both of which are consistent with Zone D average acre criterion of 100 people per acre, and single acre criterion of 300 people. The project is also located within Compatibility Zone E of March Air Reserve Base/Inland Port Airport Influence Area, where non-residential intensity is not restricted. The elevation of Perris Valley Airport's Runway 15-33 at its southerly terminus is 1,413 feet above mean sea level (AMSL). At a distance of approximately 2,643 feet from the runway to the site, Federal Aviation Administration (FAA OES) review would be required for any structures with top of roof exceeding 1,439 feet AMSL. The project site elevation is 1,420 feet AMSL, with a proposed building height of 25 feet, resulting in a top point elevation of 1,445 feet AMSL. Therefore, review of the building by the FAA Obstruction Evaluation Service (FAA OES) was required. The applicant has submitted Form 7460-1, and FAA OES has assigned Aeronautical Study No. 2022-AWP-8123-OE to this project. The aeronautical studies revealed that the proposed building would not exceed obstruction standards and would not be a hazard to air navigation provided conditions are met. Therefore, FAA OES issued a "Determination of No Hazard to Air Navigation" letter on May 27, 2022. The FAA OES conditions have been incorporated into ALUC's conditions listed below. Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33C). The nearest portion of the project is located 2,643 feet from the runway, and therefore would be subject to the above requirement. The project utilizes infiltration trenches and are permitted in Zone D. Pursuant to the study "Wildlife Hazard Management at Riverside County Airports: Background and Policy", October 2018, by Mead & Hunt, which is the basis of the brochure titled "Airports, Wildlife and Stormwater Management", such basins are suitable for use on airports and within the airport influence area. The project has been conditioned to be consistent with the basin criteria (as well as providing 48-hour draw down of the basin).

ALUC Director Paul Rull issued a determination of consistency for this project on June 1, 2022.

ZAP1100PS22 (Zone E) pertains to City of Palm Springs case No. 3.4321 MAJ (Major Architectural Review), a proposal to construct a 127,200 square foot self-storage building on 6.43 acres, located at 410 W. San Rafael Drive. The site is located within Airport Compatibility Zone E of the Palm Springs International Airport Influence Area (AIA). Within Compatibility Zone E of the Palm Springs International Airport Land Use Compatibility Plan, non-residential intensity is not restricted. The elevation of Runway 8-26 at its northerly terminus is 474.4 feet above mean sea level (AMSL). At a distance of approximately 12,066 feet from the runway to the site, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with top of roof exceeding 595.4 feet AMSL. The site's finished floor elevation is 624 feet AMSL and the proposed building height is 30 feet, for a top point elevation of 654 feet AMSL. Therefore, FAA Obstruction Evaluation Service review for height/elevation was required. Therefore, review of the buildings for height/elevation reasons by the FAA Obstruction Evaluation Service (FAAOES) is required. The applicant has submitted Form 7460-1, and FAA OES has assigned Aeronautical Study No. 2022-AWP-11003-OE to this project. A Determination of No Hazard to Air Navigation letter was issued (Aeronautical Study No. 2022-AWP-11003-OE), as the FAA OES determined that the project would not result in an impact to air navigation. The FAA OES conditions have been incorporated into ALUC's conditions listed below.

ALUC Director Paul Rull issued a determination of consistency for this project on June 14, 2022.

ZAP1069HR22 (Zone E) pertains to County of Riverside Case No. CUP220010 (Conditional Use Permit), a proposal to establish a cannabis distribution and retail facility within an existing 1,553 square foot building on 0.48 acres located at 33671 Highway 74. The site is located within Airport Compatibility Zone E of the Hemet-Ryan Airport Influence Area (AIA), which does not restrict non-residential intensity. The elevation of Runway 5-23 at its existing southwesterly terminus is approximately 1,499 feet above mean sea level (AMSL). At a distance of approximately 13,591 feet from the runway, FAA review would be required for any structures with top of roof exceeding 1,635 feet AMSL. The elevation of the project site is 1,624 feet AMSL, and the existing structure is 12 feet, for a maximum top point elevation of 1,636 feet AMSL. There is no proposal to increase the existing building height. Therefore, Federal Aviation Administration (FAA) obstruction evaluation review for height/elevation reasons is not required.

ALUC Director Paul Rull issued a determination of consistency for this project on May 27, 2022.

- B. Additionally, ALUC Director Paul Rull reviewed one local jurisdiction non-impact legislative cases pursuant to ALUC Resolution No. 2011-02, and issued determinations of consistency.

ZAP1064RG22 (March and Perris AIA) pertains to a City of Menifee Development Code Amendment (LR22-0130), a proposal amending its Zoning Code identifying a new use, Battery Energy Storage Systems (BESS), and its associated development standards, definitions and permittance. The Airport Land Use Compatibility Plan (ALUCP) criteria identifies that the storage of hazardous materials is a prohibited use in Zones B1 and B2. The ALUC has historically identified similar BESS projects as not storing hazardous material, and subsequently found those project's consistent, as long as the project meets local jurisdiction Fire and Building and Safety code regulations. The proposed Development Code Amendment contains development standards for BESS projects requiring all applicable standards of the adopted Building and Safety Code and the adopted Fire Code. With the incorporation of these regulations, it is therefore considered that BESS projects are not considered storage of hazardous materials, and not a prohibited use in the ALUCP. The proposed amendments do not involve changes in development standards or allowable land uses that would increase residential density or non-residential intensity. Therefore, these amendments have no possibility for having an impact on the safety of air navigation within airport influence areas located within the City of Menifee.

ALUC Director Paul Rull issued a determination of consistency for this project on June 2, 2022.

5.2 Update March Air Reserve Base Compatibility Use Study (CUS)
Presentation by Project Director Simon Housman or his designee.

X:\ALUC Administrative Items\Admin. 2022\Admin Item 7-14-22.doc



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

June 1, 2022

Ryan Griffiths, Project Planner
City of Perris Planning Department
101 N. D Street
Perris CA 92570

CHAIR
Steve Manos
Lake Elsinore

VICE CHAIR
Russell Betts
Desert Hot Springs

COMMISSIONERS

Vacant

John Lyon
Riverside

Richard Stewart
Moreno Valley

Steven Stewart
Palm Springs

Michael Geller
Riverside

STAFF

Director
Paul Rull

Simon Housman
Jackie Vega
Barbara Santos

County Administrative Center
4080 Lemon St., 14th Floor.
Riverside, CA 92501
(951) 955-5132

www.rcaluc.org

RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW – DIRECTOR’S DETERMINATION

File No.: ZAP1025PV22
Related File No.: CUP21-05080 (Conditional Use Permit)
APN: 330-080-006
Airport Zone: Zone D (Perris Valley); Zone E (March)

Dear Mr. Griffiths:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed City of Perris Case No. CUP21-05080 (Conditional Use Permit), a proposal to construct a modular trailer rental yard including a 6,115 square foot industrial building on 5.97 acres, located northerly of Mapes Road, westerly of Goetz Road, easterly of A street, and southerly of Artlo Avenue.

The site is located within Compatibility Zone D of the Perris Valley Airport Influence Area, where non-residential intensity is restricted to 100 people per average acre and 300 people per single acre. The project proposes a 6,115 square foot building, which consist of 2,500 square feet of warehouse area, 360 square feet of office area, 135 square feet of storage area, and 1,400 square feet of assembly area, accommodating an occupancy of 101 people, resulting in an average intensity of 21 people per acre, and a single acre intensity of 101 people, both of which are consistent with Zone D average acre criterion of 100 people per acre, and single acre criterion of 300 people. The project is also located within Compatibility Zone E of March Air Reserve Base/Inland Port Airport Influence Area, where non-residential intensity is not restricted.

The elevation of Perris Valley Airport’s Runway 15-33 at its southerly terminus is 1,413 feet above mean sea level (AMSL). At a distance of approximately 2,643 feet from the runway to the site, Federal Aviation Administration (FAA OES) review would be required for any structures with top of roof exceeding 1,439 feet AMSL. The project site elevation is 1,420 feet AMSL, with a proposed building height of 25 feet, resulting in a top point elevation of 1,445 feet AMSL. Therefore, review of the building by the FAA Obstruction Evaluation Service (FAA OES) was required. The applicant has submitted Form 7460-1, and FAA OES has assigned Aeronautical Study No. 2022-AWP-8123-OE to this project. The aeronautical studies revealed that the proposed building would not exceed obstruction standards and would not be a hazard to air navigation provided conditions are met. Therefore, FAA OES issued a “Determination of No Hazard to Air Navigation” letter on May 27, 2022. The FAA OES conditions have been incorporated into ALUC’s conditions listed below.

Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly

recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33C). The nearest portion of the project is located 2,643 feet from the runway, and therefore would be subject to the above requirement. The project utilizes infiltration trenches and are permitted in Zone D. Pursuant to the study "Wildlife Hazard Management at Riverside County Airports: Background and Policy", October 2018, by Mead & Hunt, which is the basis of the brochure titled "Airports, Wildlife and Stormwater Management", such basins are suitable for use on airports and within the airport influence area. The project has been conditioned to be consistent with the basin criteria (as well as providing 48-hour draw down of the basin).

As ALUC Director, I hereby find the above-referenced project **CONSISTENT**, with the 2011 Perris Valley Airport Land Use Compatibility Plan and the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, subject to the following conditions:

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, outdoor production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Highly noise-sensitive outdoor nonresidential uses.
 - (f) Any use which results in a hazard to flight, including physical (e.g., tall objects), visual, and electronic forms of interference with the safety of aircraft operations.

3. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property and be recorded as a deed notice.
4. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the detention basin that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the detention basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist. The infiltration basin shall be designed in accordance with all parameters identified in the Wildlife Hazard Management at Riverside County Airports: Background and Policy.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin

5. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
6. The project has been evaluated for construct a modular trailer rental yard with a 6,115 square foot industrial building consisting of 2,500 square feet of warehouse area, 360 square feet of office area, 135 square feet of storage area, and 1,400 square feet of assembly area. Any increase in building area, change in use to any higher intensity use, change in building location, or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP criteria, at the discretion of the ALUC Director.
7. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission.
8. The Federal Aviation Administration has conducted aeronautical studies of the proposed project (2022-AWP-8123-OE) and has determined that neither marking nor lighting of the structure(s) is necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 M and shall be maintained in accordance therewith for the life of the project.

9. The proposed buildings shall not exceed a height of 25 feet above ground level and a maximum elevation at top point of 1,441 feet above mean sea level.
10. The maximum height and top point elevation specified above shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in structure height or elevation shall not require further review by the Airport Land Use Commission.
11. Temporary construction equipment used during actual construction of the structure(s) shall not exceed 25 feet in height and a maximum elevation of 1,441 feet above mean sea level, unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.
12. Within five (5) days after construction of any individual building reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned or a decision is made not to construct the applicable structure(s).

If you have any questions, please contact me at (951) 955-6893.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Paul Rull, ALUC Director

Attachments: Notice of Airport in Vicinity
FAA Aeronautical Study

cc: CSLM Construction Inc. (applicant/representative/owner)
Pat Conatser, Airport Manager, Perris Valley Airport
ALUC Case File

X:\AIRPORT CASE FILES\Perris Valley\ZAP1025PV22\ZAP1025PV22.LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

**THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS**

**PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES**



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2022-AWP-8123-OE

Issued Date: 05/27/2022

Cornelius Marinescu
 CSLM Construction
 5753 Santa Ana Canyon Road
 Suite 137
 Anaheim, CA 92807

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building CSLM Construction Yard
 Location: Perris, CA
 Latitude: 33-45-28.08N NAD 83
 Longitude: 117-13-34.36W
 Heights: 1416 feet site elevation (SE)
 25 feet above ground level (AGL)
 1441 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/ lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 11/27/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

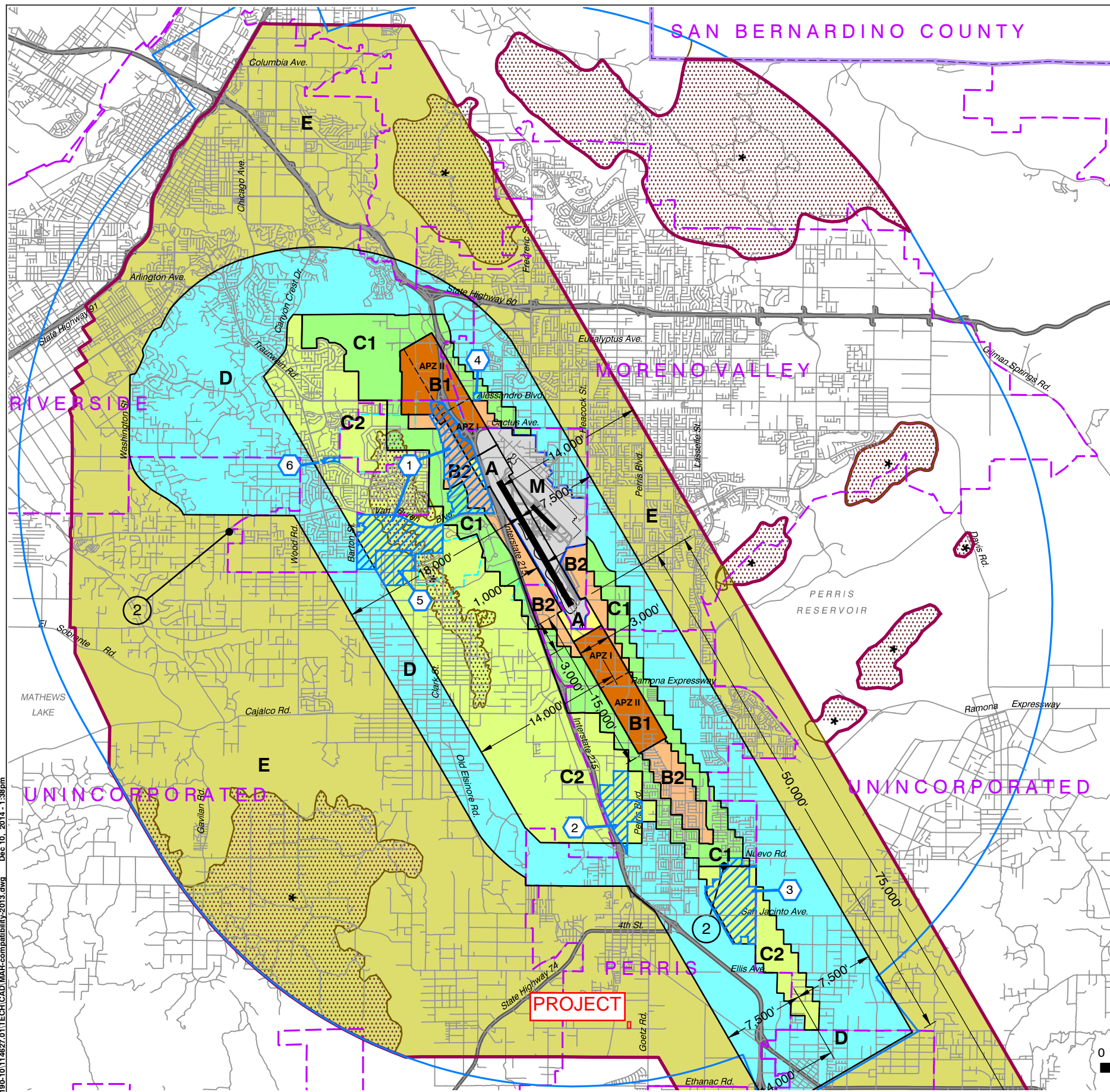
This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (206) 231-2877, or Nicholas.Sanders@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-AWP-8123-OE.

Signature Control No: 524974549-533805010

Nicholas Sanders
Technician

(DNE)



LEGEND

Compatibility Zones

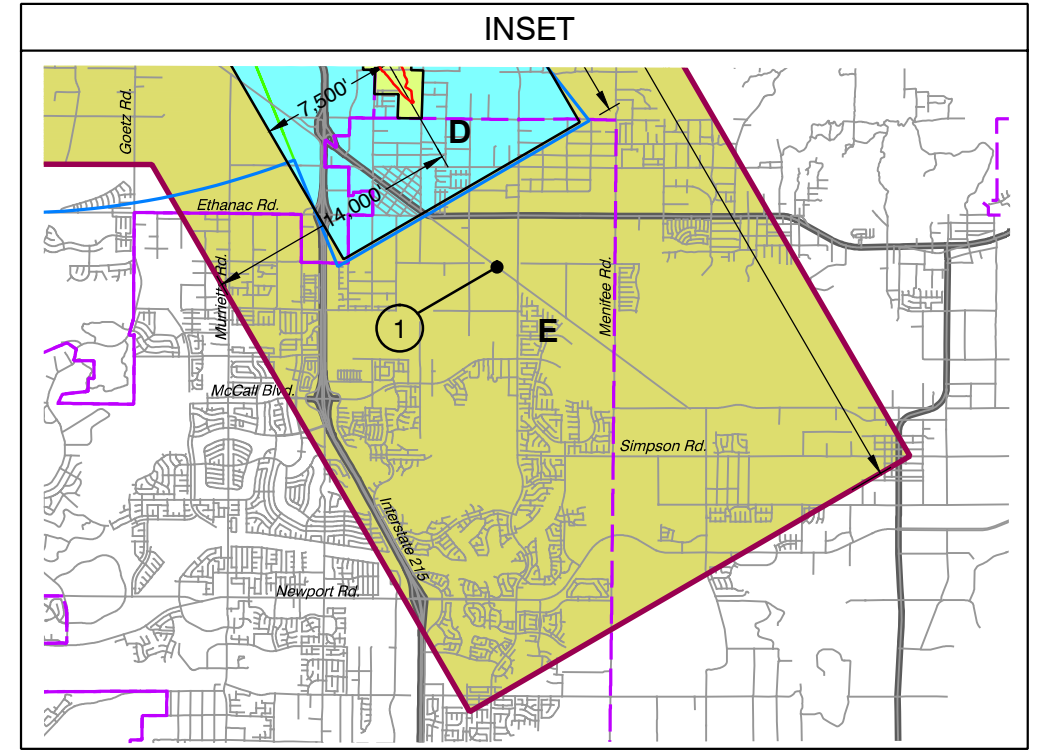
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

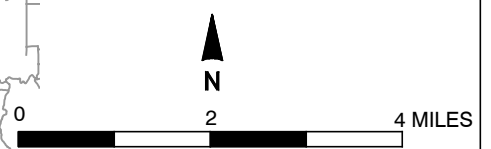
- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



**Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)**

Note:
All dimensions are measured from runway ends and centerlines.



Base map source: County of Riverside 2013

X:\18190-10\114627\01\TECH\CAD\MAR-compatibility\2013.dwg Dec 10, 2014 - 1:38pm

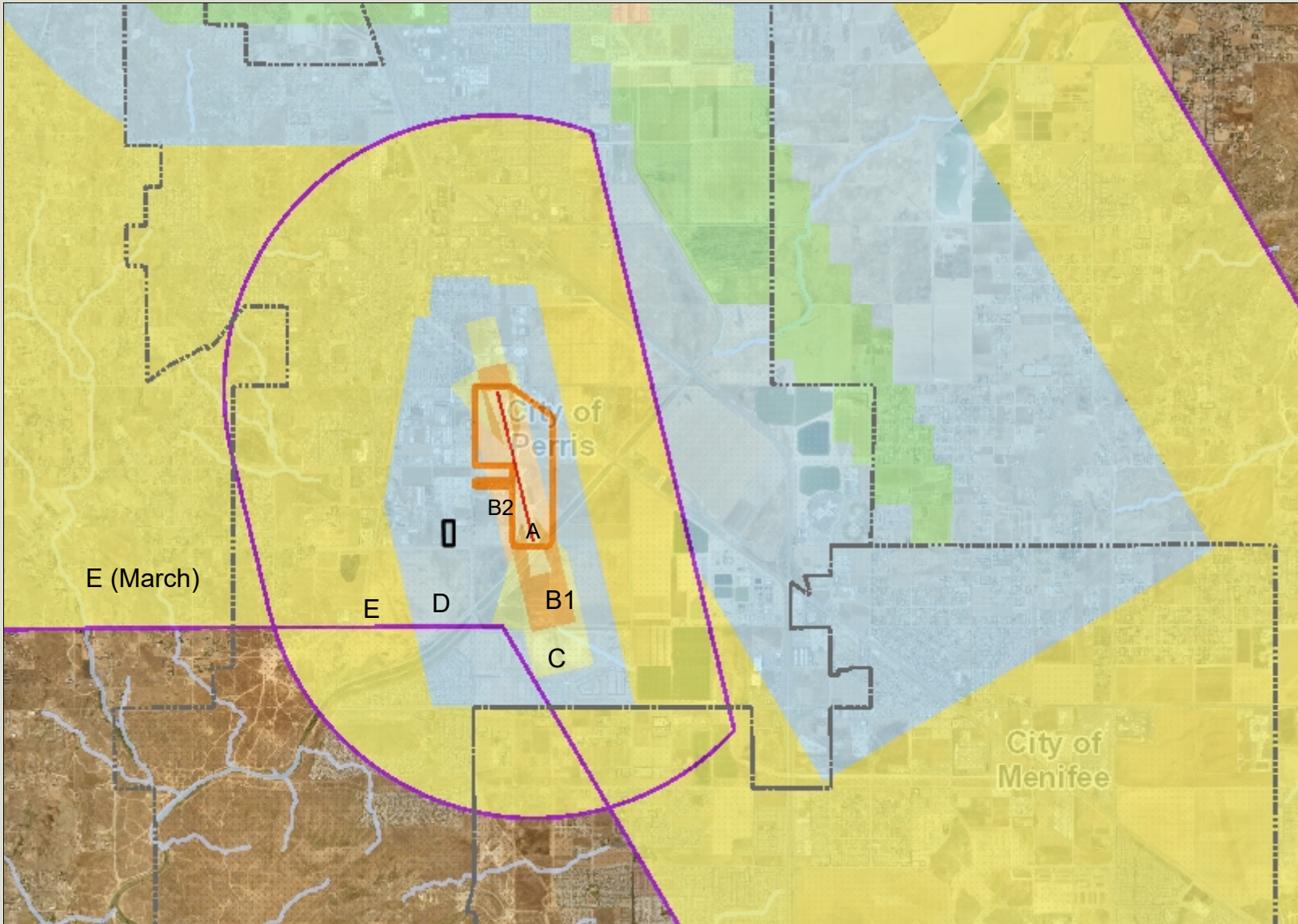
Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map MA-1

**Compatibility Map
March Air Reserve Base / Inland Port Airport**

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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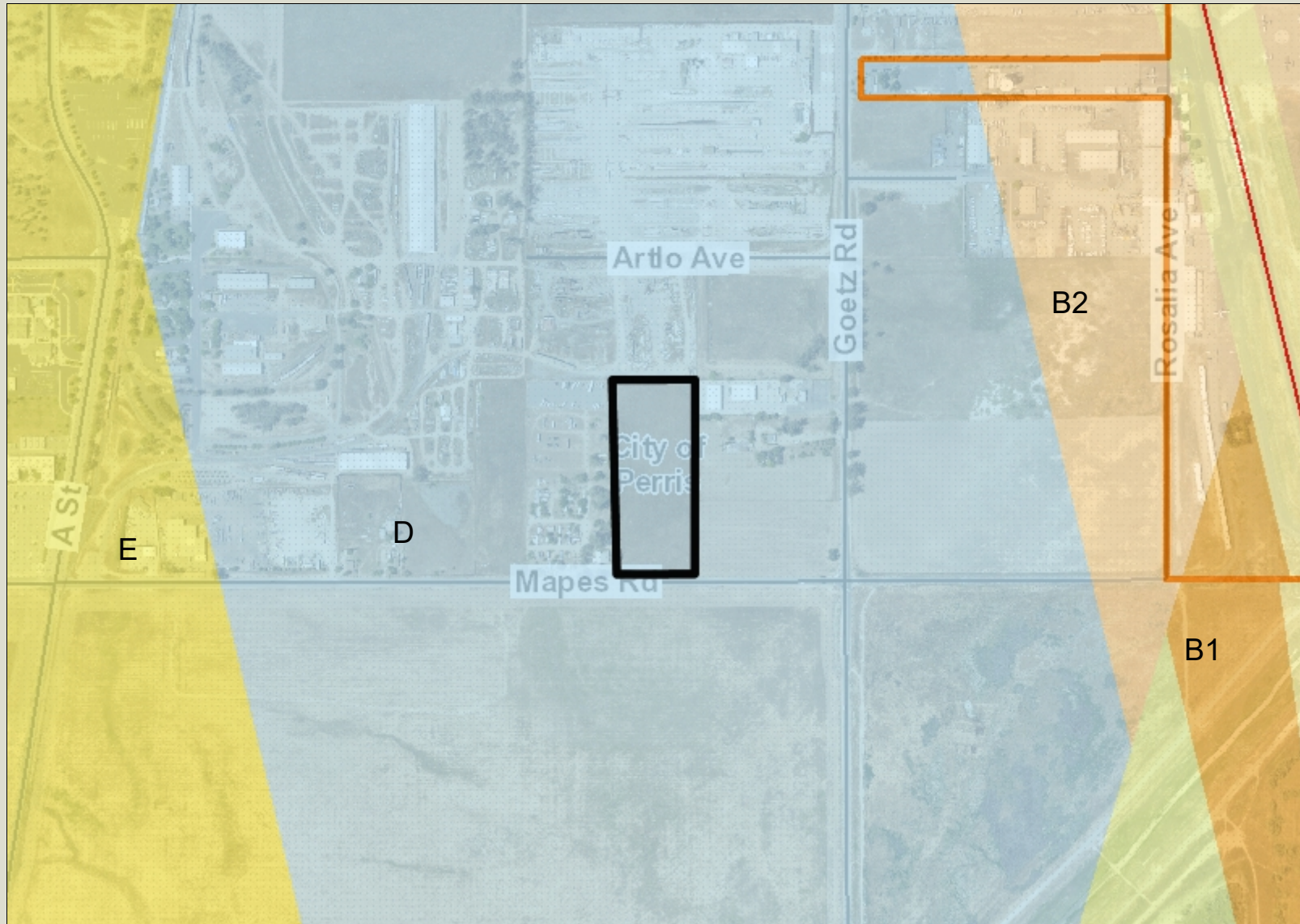
Notes



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Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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Notes

Map My County Map



Legend

- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



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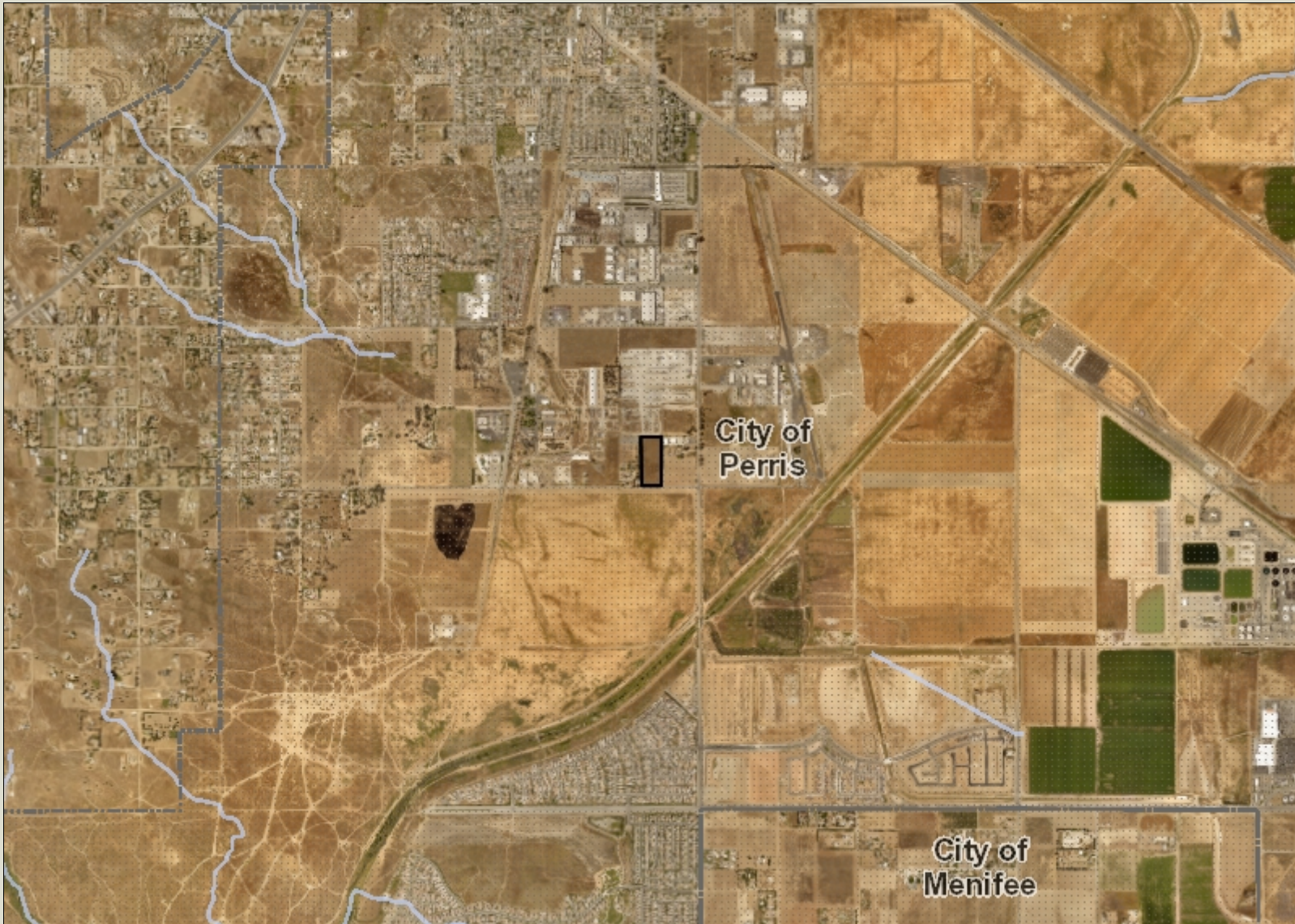
Notes

0 770 1,539 Feet


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Map My County Map



Legend

-  Blueline Streams
-  City Areas
-  World Street Map



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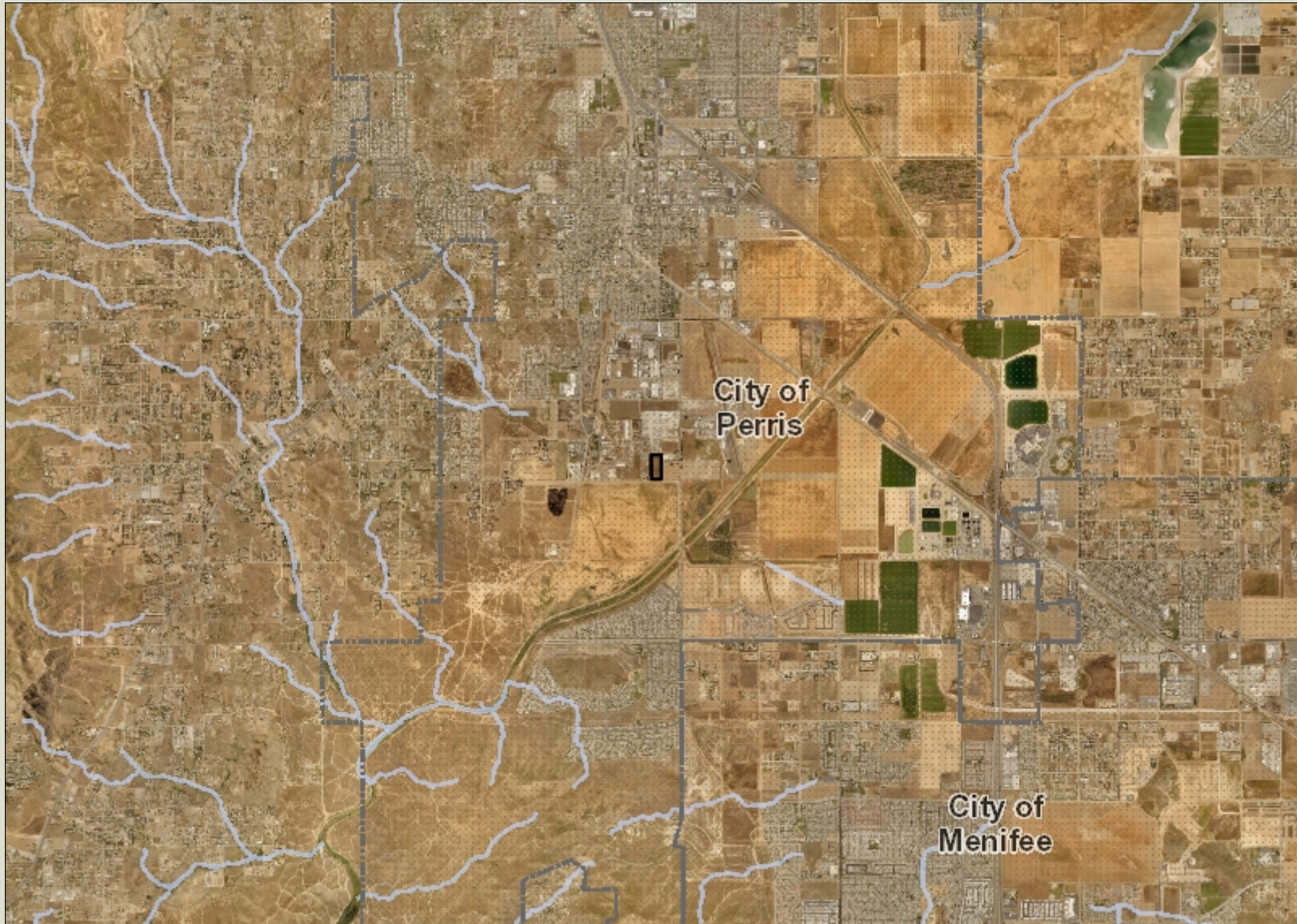


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Notes

Map My County Map



Legend

- Blue Line Streams
- City Areas
- World Street Map



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Notes



Map My County Map



Legend

- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.



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Notes

PROJECT SUMMARY

A. GENERAL INFORMATION

NOTE:
 1. TENANT SIGNAGE WILL BE SUBMITTED AT A LATER DATE.
 2. NO PROTECTED OR NATIVE TREES ARE FOUND ON THE SITE.

PROJECT ADDRESS: MAPES ROAD, PERRIS CA 92570
 APN: 330-080-006
 ZONING: LIGHT INDUSTRY
 LOT DIMENSIONS: SEE MAP
 LOT SIZE: 5.97 ACRES
 LEGAL DESCRIPTION: -
 SUBDIVISION: -
 EXISTING USE: VACANT LOT

OWNER OF RECORD:
CSLM CONSTRUCTION INC.
 Cornelius Marinescu
 5753 Santa Ana Canyon Ste #137
 Anaheim, CA 92807

APPLICANT:
T.J. Build LLC
 13841 ROSWELL AVENUE, SUITE A,
 CITY OF CHINO, CA 91710
 Tel: 951.415.9622
 Email: jimmysylee@gmail.com
 CONTACT: JIMMY LEE

SCOPE OF WORK :

DEVELOP THE PROPERTY WITH AN ONE-STORY OFFICE/WAREHOUSE BUILDING WITH ASSOCIATED PARKING ALONG WITH A SCREEN AREA TO STORE MODULAR TRAILERS.

BUILDING(S) INFORMATION :

PROJECT PROPOSED: NEW 1-STORY WAREHOUSE/OFFICE
 BLDG. OCCUPANCY: GROUP S-1 (2,500 SF)
 GROUP B (3,615 SF)

CONSTRUCTION TYPE: TYPE VB, (W/ R-13D FIRE SPRINKLERS)
 PROPOSED BLDG HT: 1-STORY / 24'-6"
 FLOOR AREA:

1,440 SF. LOBBY/RECEPTION/MEETING
 2,175 SF. OFFICE
 2,500 SF. WAREHOUSE
 6,115 SF.

TOTAL FLOOR AREA : 6,115 SF.

OCCUPANT LOAD DETERMINATION PER TABLE 1004.1.2 :

OCCUPANT LOAD:
 OFFICE (B): 3,615 SF/ 200=18.075
 WAREHOUSE (S): 2,500 SF/ 5,000=0.5

REQUIRED AND PROPOSED YARD SETBACKS :

YARD LOCATION	REQUIRED	PROPOSED
FRONT YARD:	15'-0" AVERAGE	15'-0"
SIDE YARD:	0'	10'-11" MIN.

LOT COVERAGE INFORMATION :

	AREA	COVERAGE(%)
GROSS LOT SIZE:	260,053 Sq.Ft.	
NET LOT SIZE (AFTER DEDICATION):	254,636 Sq.Ft.	100%
BUILDING:	6,115 Sq.Ft.	2.40 %
OUTDOOR STORAGE AREA:	186,736 Sq.Ft.	73.33 %
PARKING/CIRCULATION:	35,010 Sq.Ft.	13.75 %
LANDSCAPE:	26,775 Sq.Ft.	10.52 %

PARKING SPACES ANALYSIS :

	REQUIRED	PROVIDED	FORMULA
OFFICE:	12.05	18	3,615 SF / 300 = 12.05
WAREHOUSE:	5.00	10	2,500 SF / 500 = 5.00
(TOTAL):	17.05	28	

28 PARKINGS = 24 STANDARD + 4 HANDICAP

B. CODES INFORMATION

- 2019 CALIFORNIA BUILDING CODE- TITLE 24, PART 2
- 2019 CALIFORNIA ELECTRICAL CODE- TITLE 24,PART3
- 2019 CALIFORNIA MECHANICAL CODE-TITLE 24, PART 4
- 2019 CALIFORNIA PLUMBING CODE-TITLE 24, PART 5
- 2019 CALIFORNIA ENERGY CODE-TITLE 24, PART 6
- 2019 CALIFORNIA FIRE CODE-TITLE 24, PART 6
- 2019 CALIFORNIA GREEN BUILDING STANDARDS CODE
- Applicable Federal, State, County and City Ordinances

SHEET SCHEDULE

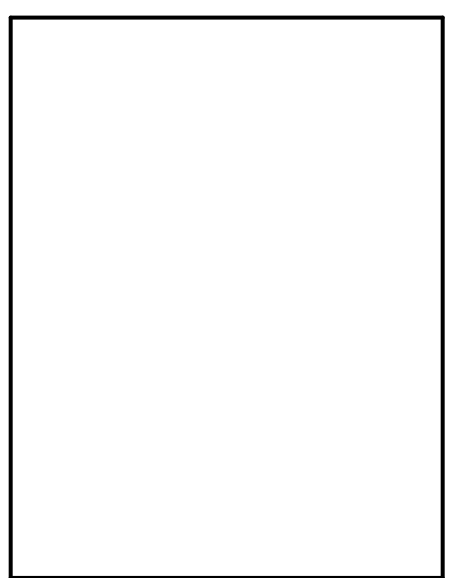
SHEET	PLAN TITLE	REVISION	LAST ISSUE
A0	SITE PLAN / PROJECT DATA	-	-
A0.1	ENLARGED SITE PLAN	-	-
A0.2	FIRE ACCESS PLAN	-	-
A1	PROPOSED BUILDING FLOOR PLANS	-	-
A2	PROPOSED BUILDING ELEVATIONS	-	-
A2C	PROPOSED BUILDING ELEVATIONS-COLOR	-	-
L1	PROPOSED CONCEPTUAL LANDSCAPE PLAN	-	-
C1	PROPOSEDPRELIMINARY GRADING PLAN	-	-

CSLM CONSTRUCTION INC.
 MAPES ROAD
 PERRIS, CA 92570

APN 330-080-006
 CASE NO. CUP-21-05080

REVISIONS:

NO.	DATE	DESCRIPTION
1		



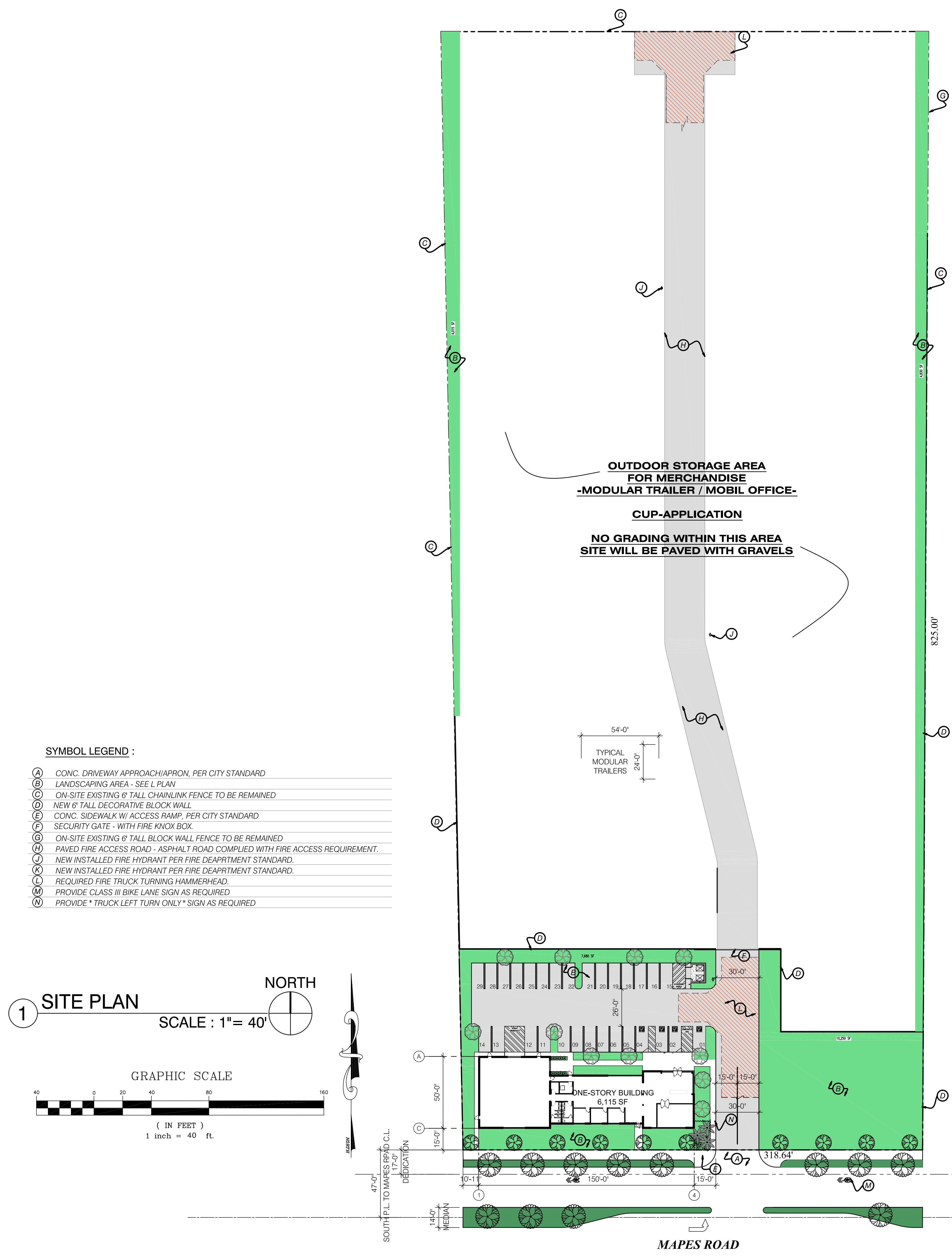
PROJECT DATA
 OVERALL SITE PLAN

CUP-21-05080

DATE: Feb 28, 2022
 PRINTED: AS NOTED

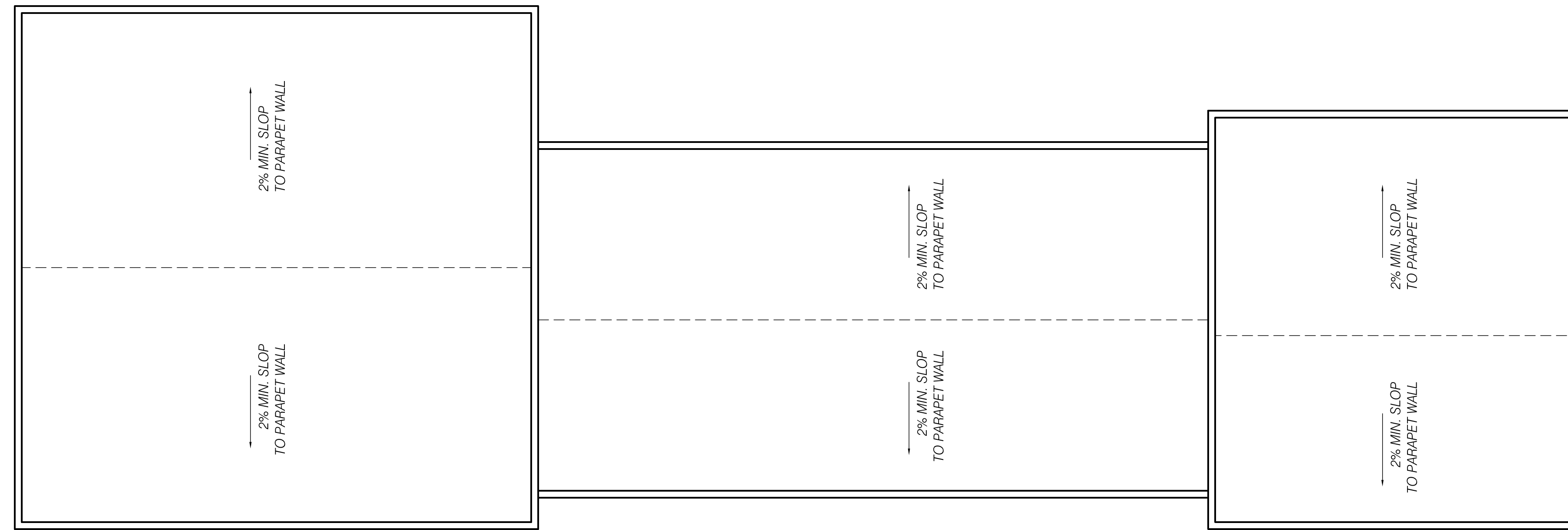
DRAWN: J. L.
 JOB NO. 2118

SHEET A0

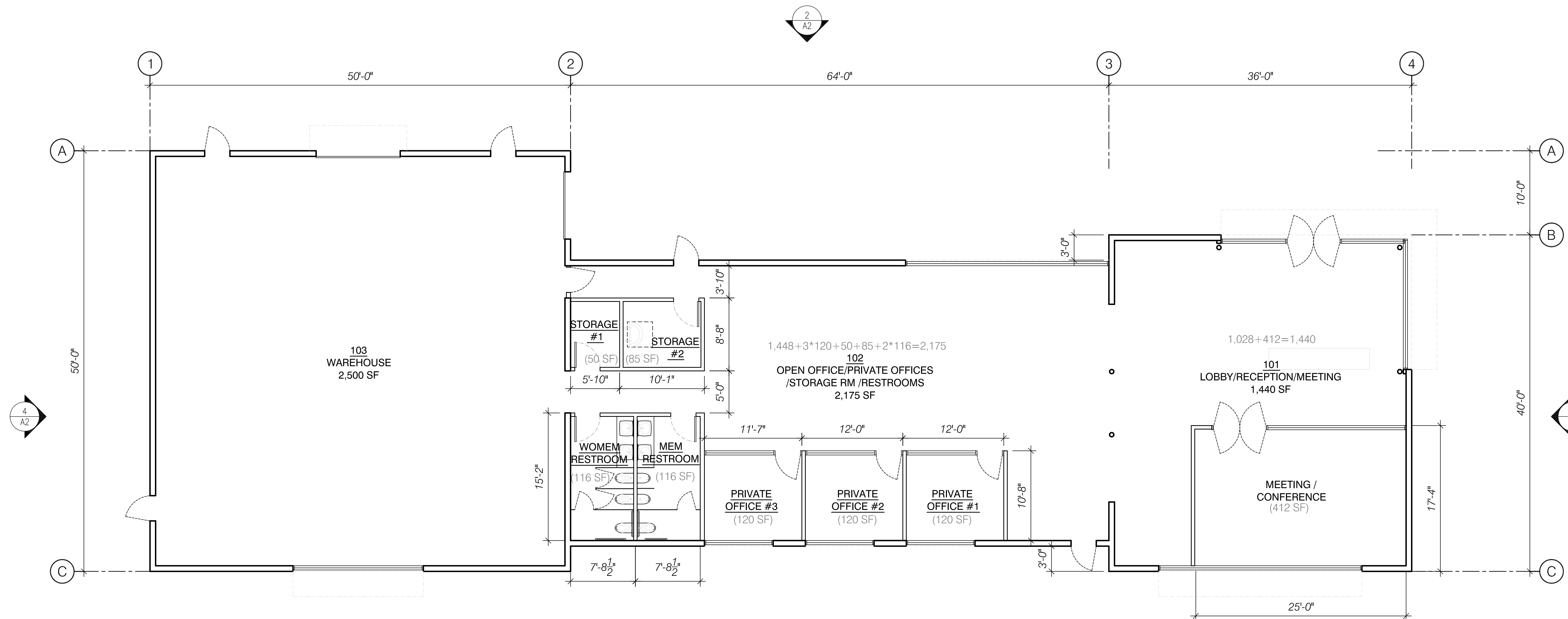
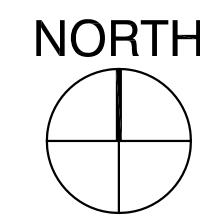


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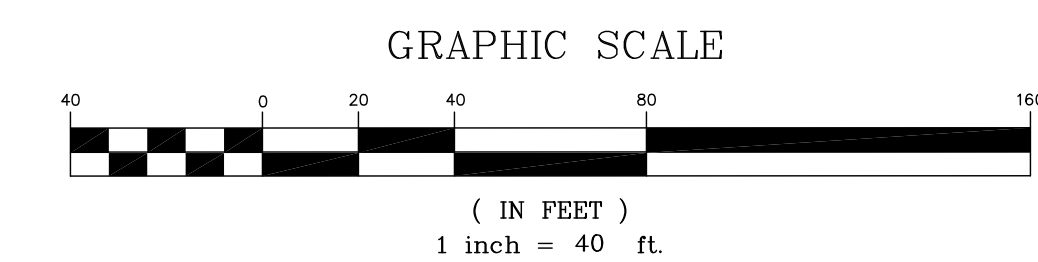
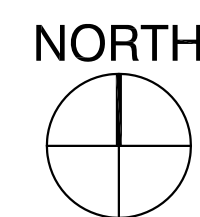
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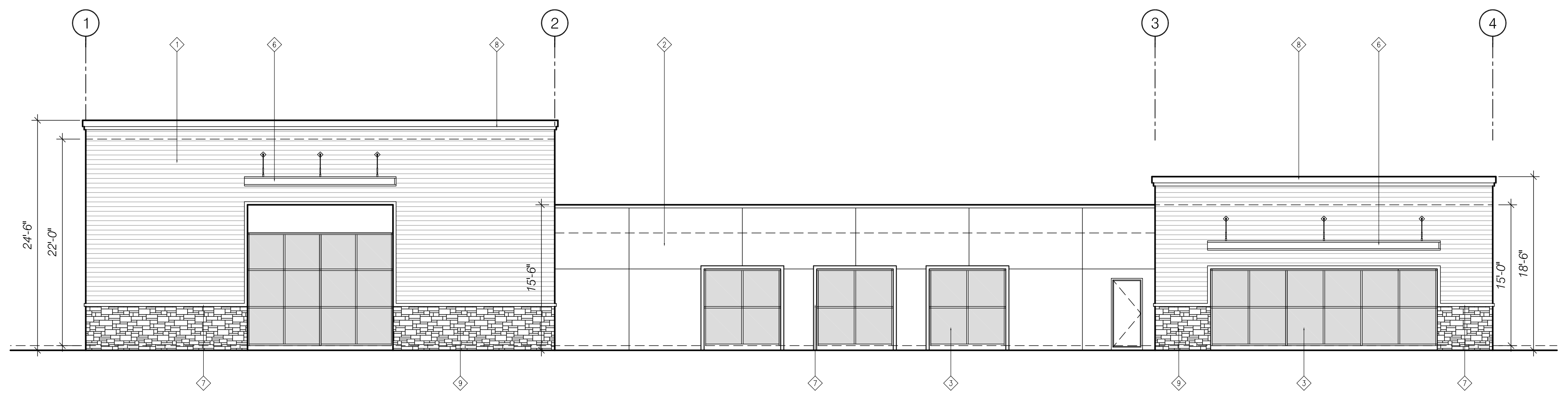


1 ROOF PLAN
SCALE : 1/8" = 1'-0"

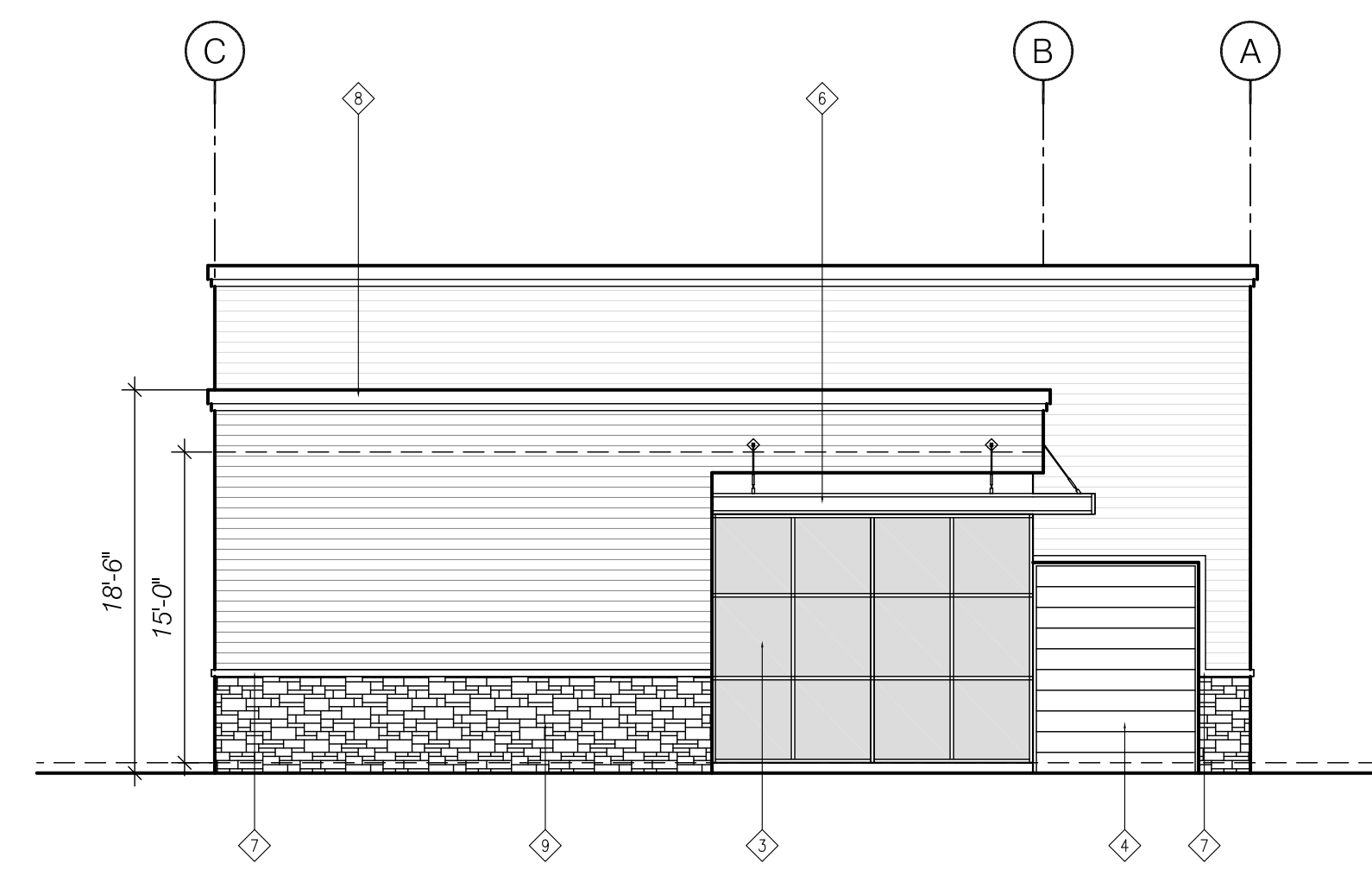


2 FLOOR PLAN
SCALE : 1/8" = 1'-0"





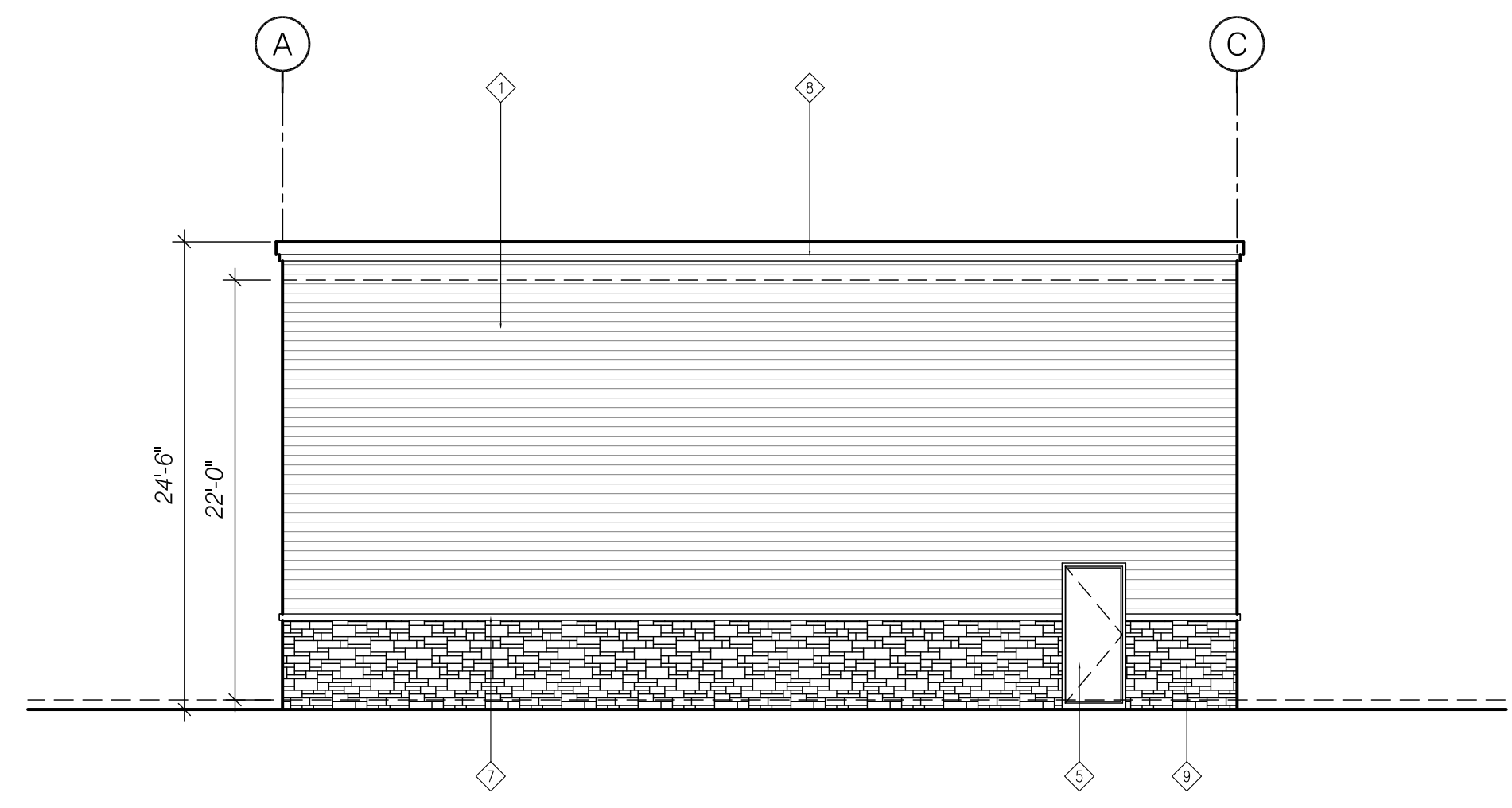
3 SOUTH ELEVATION
 SCALE : 1/8"=1'-0"



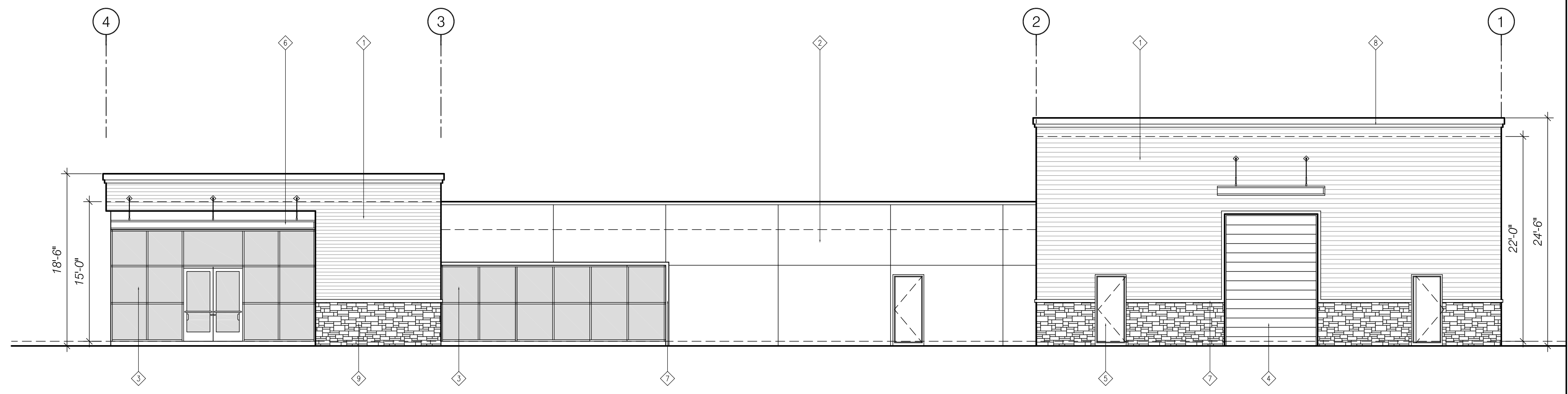
1 EAST ELEVATION
 SCALE : 1/8"=1'-0"

MATERIAL LEGEND

1.	* METLSPAN SANTA FE * SYSTEM (42" HORIZ.) FLUTED
2.	* METLSPAN SANTA FE * SYSTEM (42" HORIZ.) SMOOTH.
3.	MEDIUM REFLECTIVE GLASS.
4.	METAL ROOL-UP DOOR - CLOOR MATCHES WALL.
5.	METAL EXIT DOOR.
6.	ALUMINUM AWING -PAINTED DARK BROWN
7.	ACCENT WINDOW TRIMS - PAINTED DARK BROWIN
8.	BUILDING PARAPET WALL TRIM - PAINTED DARK BROWN
9.	CULTURE STONE VENEER BASE



4 WEST ELEVATION
 SCALE : 1/8"=1'-0"

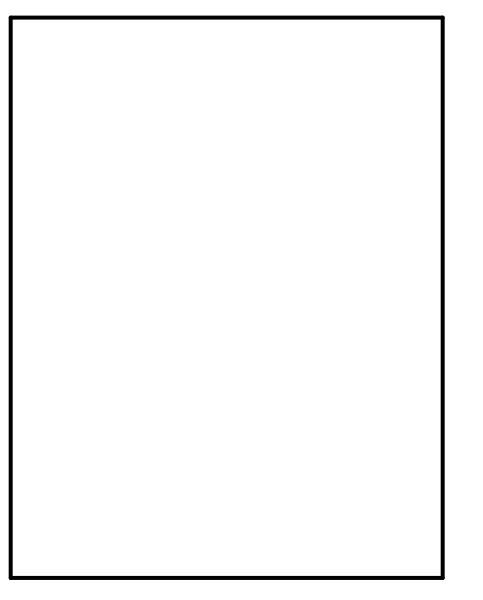


2 NORTH ELEVATION
 SCALE : 1/8"=1'-0"

CSLM CONSTRUCTION INC.
 MAPES ROAD
 PERRIS, CA 92570
 APN 330-080-006
 CASE NO. CUP-21-05080

REVISIONS:

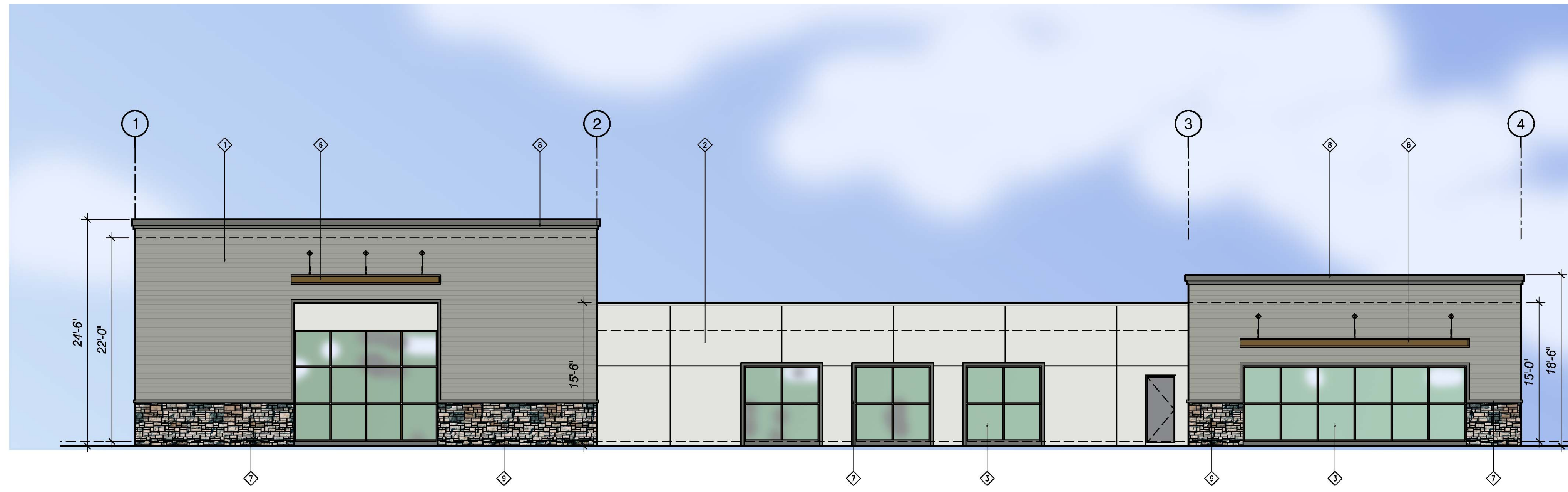
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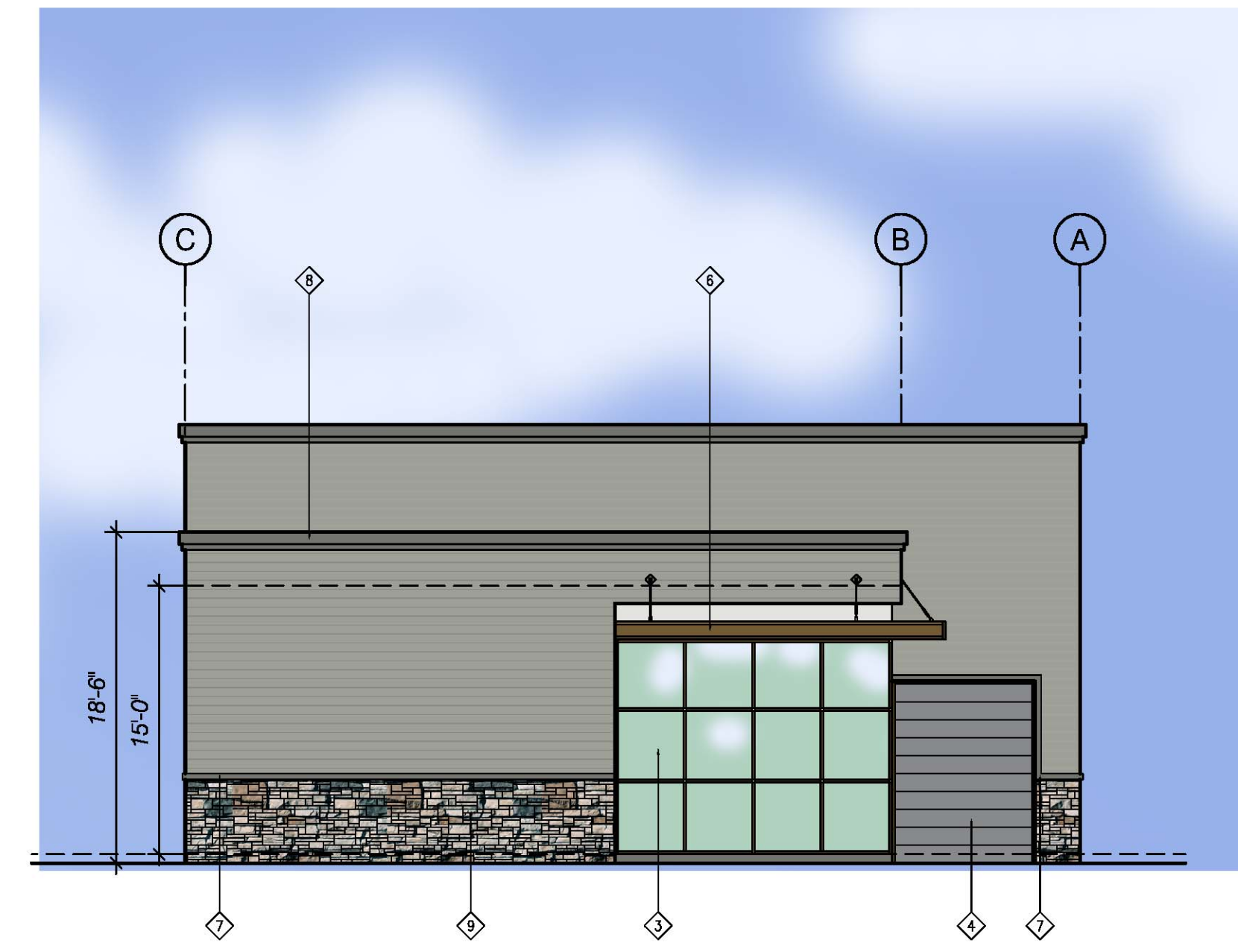
BUILDING ELEVATIONS

CUP-21-05080

DATE PRINTED:	
DATE:	Feb 28, 2022
SCALE:	AS NOTED
DRAWN:	J. L.
JOB NO.:	2118
SHEET:	



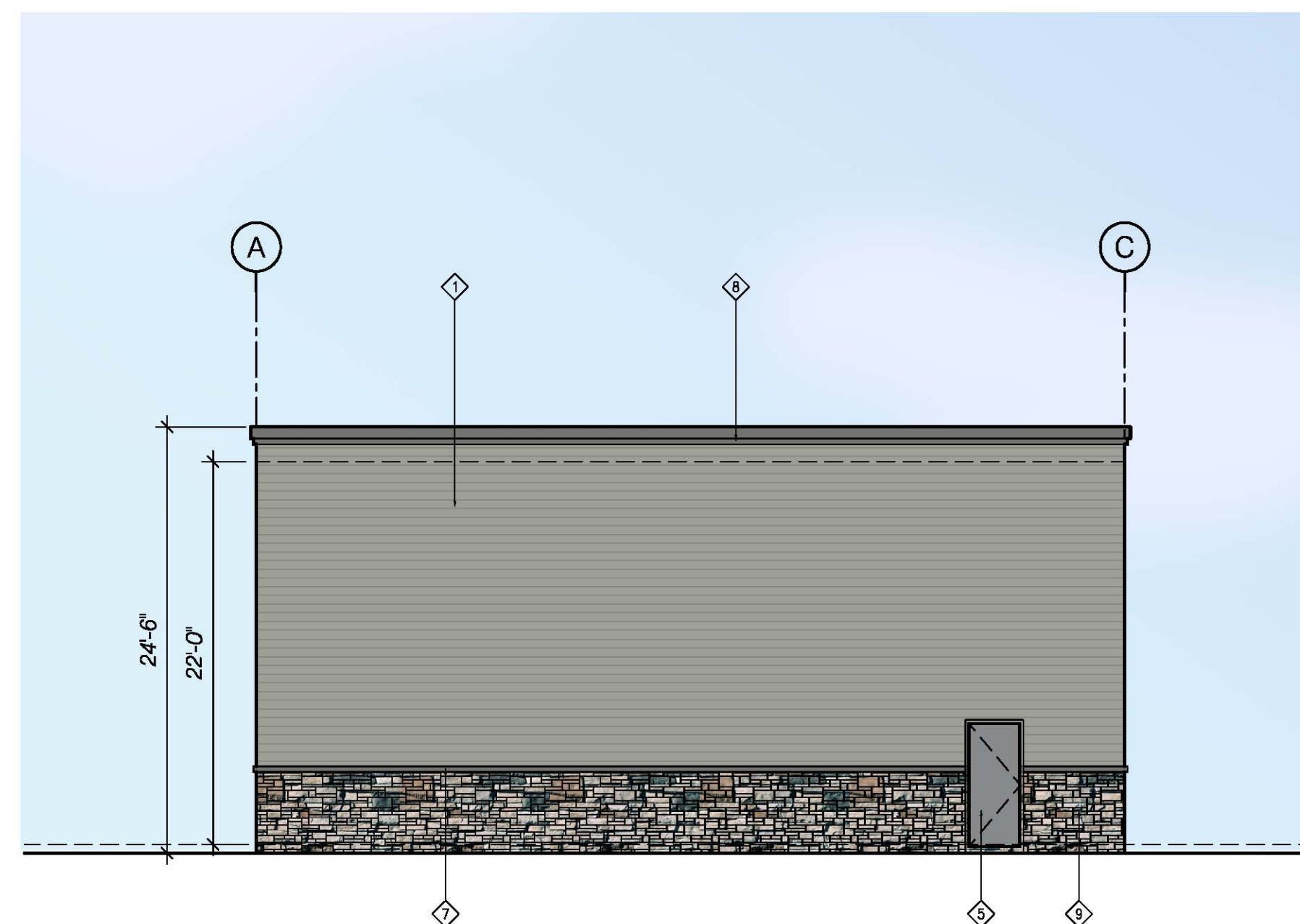
3 SOUTH ELEVATION
SCALE : 1/8"=1'-0"



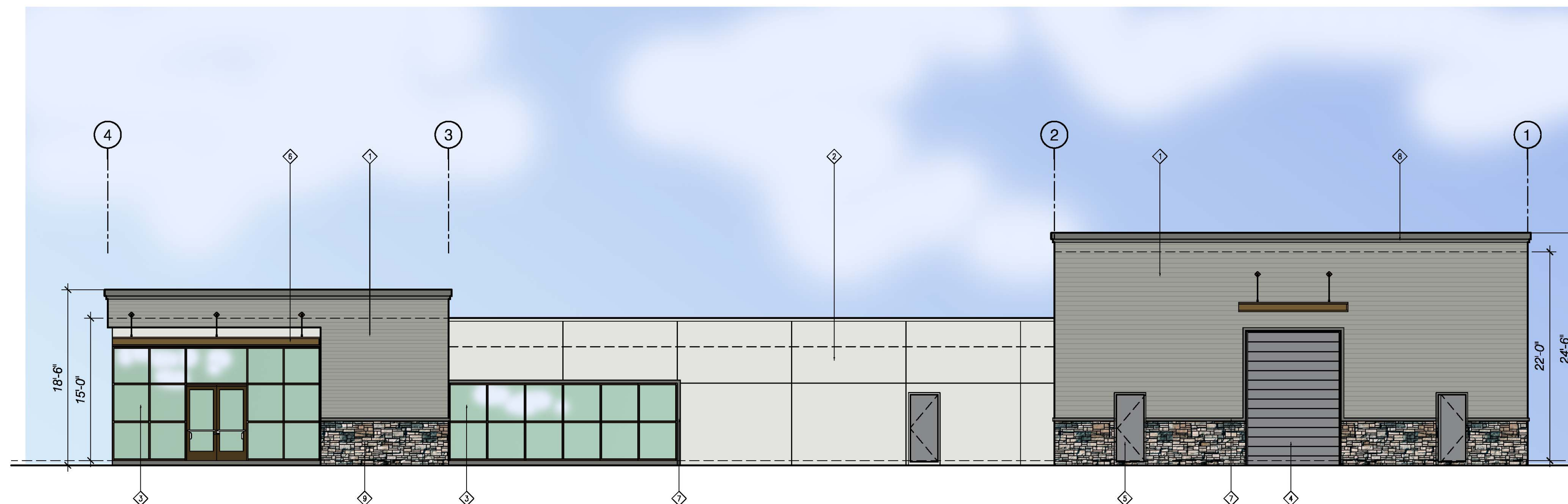
1 EAST ELEVATION
SCALE : 1/8"=1'-0"

MATERIAL LEGEND

- 1. * METLSPAN SANTA FE * SYSTEM (42" HORIZ.) FLUTED
- 2. * METLSPAN SANTA FE * SYSTEM (42" HORIZ.) SMOOTH.
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- 9. CULTURE STONE VENEER BASE



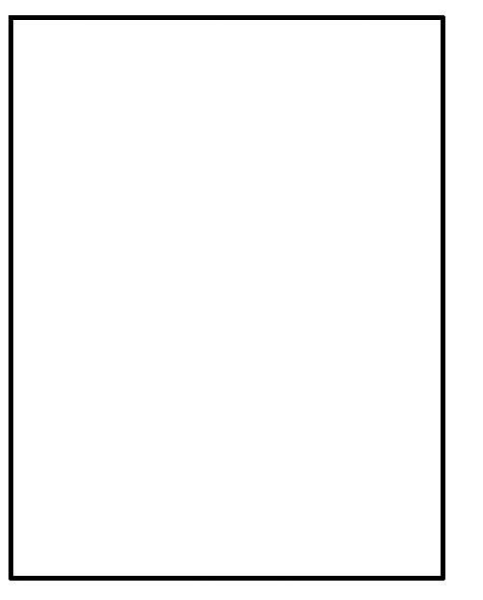
4 WEST ELEVATION
SCALE : 1/8"=1'-0"

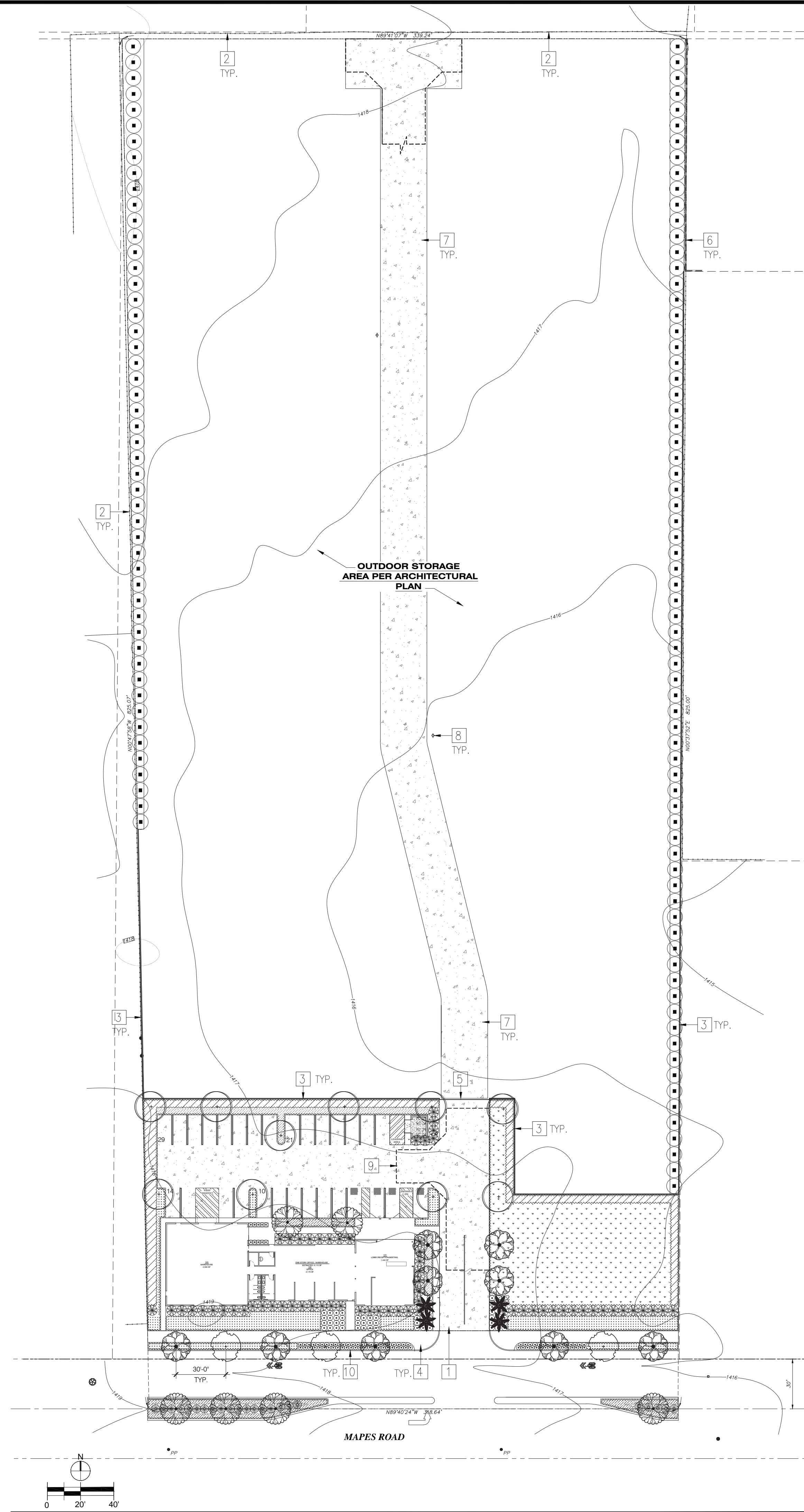


2 NORTH ELEVATION
SCALE : 1/8"=1'-0"

REVISIONS:

1	





A LANDSCAPE PLAN 1"=40'

HARDSCAPE LEGEND

KEY NOTE	COLOR / FINISH
1	CONC. DRIVEWAY APPROACH/APRON PER ARCH.
2	ON-SITE EXISTING 6' TALL CHAINLINK FENCE TO BE REMAINED
3	NEW 6' TALL DECORATIVE BLOCK WALL, PER ARCH.
4	CONC. SIDEWALK W/ ACCESS RAMP, PER ARCH.
5	SECURITY GATE - WITH FIRE KNOX BOX PER ARCH.
6	ON-SITE EXISTING 6' TALL BLOCK WALL FENCE TO REMAIN
7	PAVED FIRE ACCESS ROAD - ASPHALT ROAD PER ARCH.
8	NEW INSTALLED FIRE HYDRANT
9	FIRE TRUCK TURNING HAMMERHEAD.
10	CONCRETE CURB, PER ARCH.

PLANTING LEGEND

TREE SYMBOL	QNTY	SIZE	BOTANICAL NAME/ COMMON NAME	WATER USE PLANT FACTOR
T1	3	24" BOX	RHUS LANCEA AFRICAN SUMAC	LOW .5
T2	9	24" BOX	CHITALPA 'PINK DAWN' PINK DAWN CHITALPA	LOW .5
T3	6	24" BOX	LAGERSTROEMIA INDICA 'MUSKOGEE' CRAPE MYRTLE	MODERATE .5
T4	4	8' BTH	PHOENIX DACTYLIFERA DATE PALM	MODERATE .5
T5	10	15 BOX	RHUS LANCEA AFRICAN SUMAC	LOW .5

SHRUB

S1	91	5 GAL	SALVIA GREGII 'FURMAN'S RED' AUTUMN SAGE	LOW .2
S2	23	5 GAL	HESPERALOE PARVIFLORA RED YUCCA	LOW .2
S3	66	5 GAL	WESTRINGIA FRUTICOSA COASTAL ROSEMARY	LOW .2
S4	54	5 GAL	LANTANA MONTEVIDENSIS 'NEW GOLD' NEW GOLD TRAILING LANTANA	LOW .2
S5	123	5 GAL	PODOCARPUS GRACILIOR FERN PINE	LOW .2
S6	87	5 GAL 6" O.C.	OLEA EUROPAEA 'MONTRA' LITTLE OLLIE DWARF OLIVE	LOW .2

GROUND COVER

GC1	TRIANGULAR SPACING	3' O.C. 1 GAL	LANTANA MONTEVIDENSIS 'ALBA' WHITE TRAILING LANTANA	LOW .2
GC2	TRIANGULAR SPACING	3' O.C. 1 GAL	ROSEMARINUS OFFICINALIS 'PROSTRATUS' DWARF ROSEMARY	LOW .2
GC3	TRIANGULAR SPACING	4' O.C. 1 GAL	MYOPORUM PARVIFOLIUM N.C.N.	LOW .2

NOTE:
 1. CONTRACTOR BID THE QUANTITY AND SIZE OF TREES OR SHRUBS. OWNER AND DESIGNER MAY CHANGE THE TYPE OF SHRUBS AND ARRANGEMENT OF SHRUBS PER ACTUAL SITE CONDITION TO ACHIEVE THE BEST DESIGN APPEARING. ANY ALTERNATE PLANTS MUST MATCH WATER USE REQUIREMENTS OF THE HYDROZONE, PER APPROVED IRRIGATION PLAN.
 2. REFER TO ARCHITECTURAL SITE PLAN AND DETAILS FOR HARDSCAPE.
 3. PROVIDE 3" THICK BARK MULCH UNDER ALL GROUND COVER / SHRUB TYP., SHREDDED WOOD CHIPS 1"-3" LENGTH, 3/8"-5/8" DIAMETER.

OWNER/ DEVELOPER:

CSLM CONSTRUCTION INC.

PHONE: (714) 863-7686

EMAIL: CMMODULAR@YAHOO.COM

SHEET INDEX:

- L-1 LANDSCAPE SITE PLAN
- L-2 LANDSCAPE ENLARGEMENT PLAN

REVISIONS	BY
CD 12-08-21	
02-28-22 04-12-22	

JYC
 LANDSCAPE
 ARCHITECTURE
 PHONE: (951) 817-6825
 Email: jennytye@yahoo.com



CONCEPTUAL LANDSCAPE PLAN
 FOR "CUP-21-05080"

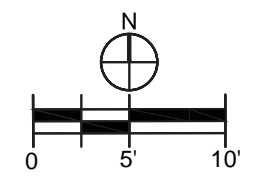
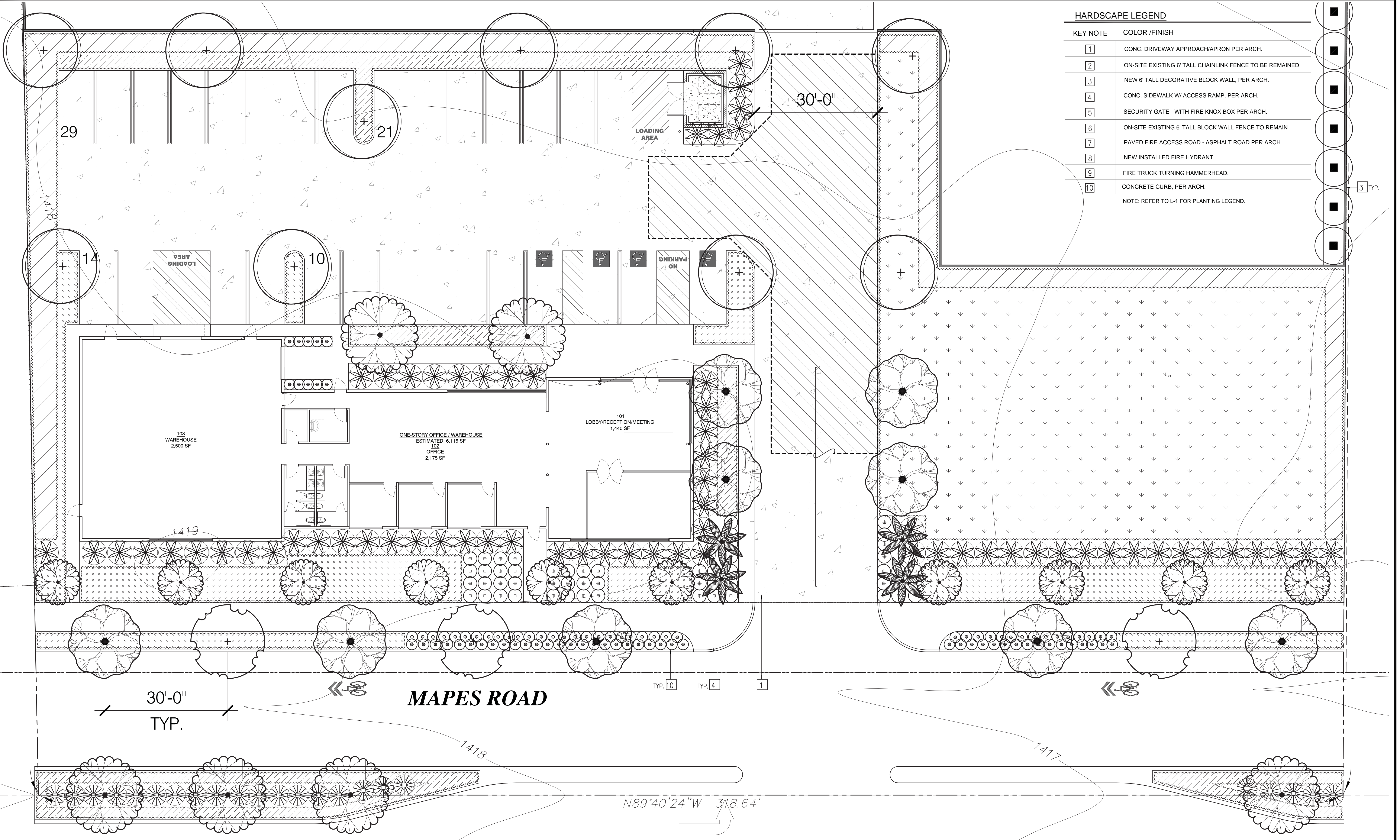
PROJECT NAME:
 CSLM CONSTRUCTION INC.
 MAPES ROAD
 PERRIS, CA 92570
 APN 330-080-006

DATE	04-12-22
SCALE	PER PLAN
DRAWN	
JOB	211022
SHEET	

HARDSCAPE LEGEND

KEY NOTE	COLOR /FINISH
1	CONC. DRIVEWAY APPROACH/APRON PER ARCH.
2	ON-SITE EXISTING 6' TALL CHAINLINK FENCE TO BE REMAINED
3	NEW 6' TALL DECORATIVE BLOCK WALL, PER ARCH.
4	CONC. SIDEWALK W/ ACCESS RAMP, PER ARCH.
5	SECURITY GATE - WITH FIRE KNOX BOX PER ARCH.
6	ON-SITE EXISTING 6' TALL BLOCK WALL FENCE TO REMAIN
7	PAVED FIRE ACCESS ROAD - ASPHALT ROAD PER ARCH.
8	NEW INSTALLED FIRE HYDRANT
9	FIRE TRUCK TURNING HAMMERHEAD.
10	CONCRETE CURB, PER ARCH.

NOTE: REFER TO L-1 FOR PLANTING LEGEND.



A | LANDSCAPE ENLARGEMENT PLAN 1"=10'

REVISIONS	BY
CD	12-08-21
02-28-22	04-12-22

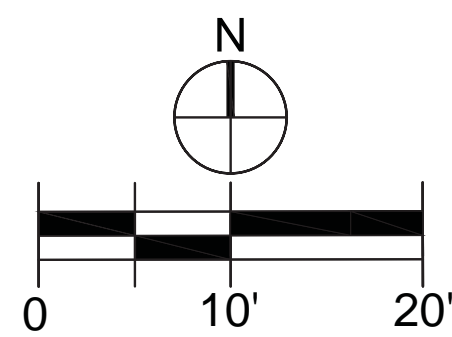
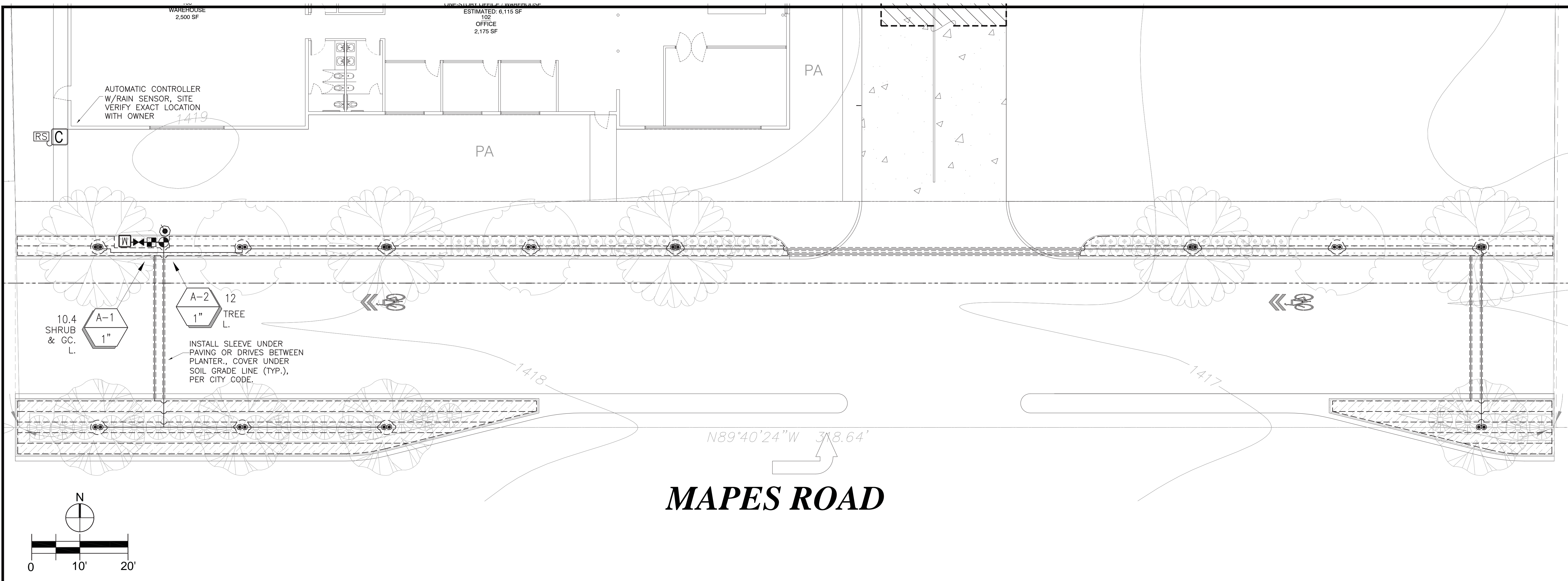


JYC
LANDSCAPE
ARCHITECTURE
PHONE: (951) 317-6825
Email: jerryjyc@yahoo.com

SHEET TITLE:
LANDSCAPE PLAN
FOR "CUP-21-05080"

PROJECT NAME:
CSLM CONSTRUCTION INC.
MAPES ROAD
PERRIS, CA 92570
APN 330-080-006

DATE 04-12-22
SCALE PER PLAN
DRAWN
JOB 211022
SHEET **L-2**
OF 2 SHEETS



A | OFF-SITE IRRIGATION PLAN 1"=20'

WATER EFFICIENT LANDSCAPING WORKSHEET							
Mapes Road, Perris, CA 92570, APN 330-080-006							
This worksheet is filled out by the project applicant and it is a required element of the Landscape Documentation Package.							
Reference Evapotranspiration (Eto) Rate = 55.6 inches per year							
The Eto for the area is based on the State Department of Water Resource, Reference Evapotranspiration rate (Eto) for the City of Riverside - Appendix A of the State MWVLO							
Hydrozone# & Planting Description	Plant Factor (PF)	Irrigation Method ^b	Irrigation Efficiency (IE) ^c	ETAF (PFIE)	Landscape Area (sq. ft.)	ETAF x Area	Estimated Total Water Use (ETWU) ^d
Regular Landscape Areas							
A-1	0.2	Drip	0.81	0.25	2,591.0	639.8	22,053.6
A-2	0.2	Spray	0.75	0.27	108.0	28.8	992.8
					Totals	2,699.0	668.6
					(A)	(B)	
Special Landscape Areas (SLAs) - Includes areas irrigated with recycled water							
None				1	--	--	
					Totals	(C)	(D)
						ETWU Total	23,046.4
						Maximum Allowed Water Allowance (MAWA)^e	51,172.0
^a Hydrozone #/Planting Description	^b Irrigation Method	^c Irrigation Efficiency	^d ETWU (Annual Gallons Required) = Eto x 0.62 x ETAF x Area				
E.g. overhead spray	or drip	0.75 for spray head 0.81 for drip	where 0.62 is a conversion factor that converts acre-inches per acre per year to gallons per square foot per year				
1) front lawn							
2) low water use plantings							
^e MAWA (Annual Gallons Allowed) = (Eto) (0.62) [(ETAF x LA) + ((1-ETAF) x SLA)]							
where 0.62 is a conversion factor that converts acre-inches per acre per year to gallons per square foot per year. LA is the total landscape area in square feet, SLA is the total special landscape area in square feet, and ETAF is .55 for residential areas and 0.45 for non-residential area							
ETAF Calculations							
Regular Landscape Areas							
Total ETAF x Area	(B)	668.6					
Total Area	(A)	2,699.0	Average ETAF for Regular Landscape Areas must be 0.55 or below for residential areas, and 0.45 or below for non-residential areas.				
Average ETAF	B ÷ A	0.25					
All Landscape Areas							
Total ETAF x Area	(B+D)	669					
Total Area	(A+C)	2,699					
Sitewide ETAF	(B+D) ÷ (A+C)	0.25					

IRRIGATION LEGEND

SYMBOLS	DESCRIPTION	MFG. & MODEL	DETAILS/ SHEET				
P.O.C.	POINT OF CONNECTION						
W	5/8" WATER METER (CITY WATER SOURCE PHONE:(909)395-2678)	INSTALL NEW 'T' FOR LANDSCAPE SITE VERIFY METER SIZE	-				
K	GATE VALVE	SITE VERIFY. 'NIBCO' T-113 OR EQUAL.	02/ L4				
C	AUTOMATIC CONTROLLER	RAINBIRD ESP-TM2, 12-STATION, LNK WIFI MODULE, WALL MOUNTED CONTROLLER	03/ L4				
RS	RAIN SENSOR	RAINBIRD WR-2 WIRELESS RAIN SENSOR SITE VERIFY LOCATION.	04/ L4				
---	SLEEVE	PURPLE SCH-40 PVC, SIZE X2 PIPE SIZE	05/ L4				
---	PRESSURE MAINLINE	PURPLE PVC IRRIGATION MAINLINE, 2" AND ABOVE - CL315 PVC, UP TO 1.5" - SCH40 PVC, 18" MINIMUM COVER	05/ L4				
---	NON-PRESSURE LATERAL LINE	PURPLE SCH-40 PVC, SHALL BE 3/4" UNLESS OTHERWISE NOTED ON PLAN - 12" MINIMUM COVER	05/ L4				
	NOT SHOWN	UF DIRECT BURIAL CONTROL WIRE WITH WATERPROOF CONNECTIONS	-				
⊙	1" RAIN BIRD 3RC QUICK COUPLING VALVE W/ ACME THREAD FOR RECYCLE WATER IN CITY APPROVED BOX		08/ L4				
⊕	RAIN BIRD PEB REMOTE CONTROL VALVE IN CITY APPROVED BOX, SIZE PER PLAN		06/ L4				
⊞	DRIP VALVE, RAIN BIRD XCZ-75-PRF 3/ 4" (5 TO 5 GPM), XCZ-100-PRF 1" (3 TO 15 GPM) CONTROL ZONE KIT 'PURPLE LID' IN ROUND VALVE BOX,		07/ L4				
⊚	DRIP MANUAL FLUSH VALVE WITH MINIMUM 6" TUBING, INSTALL ONE PER DRIP VALVE AT LOW POINT OF EACH ZONE		13/ L4				
IRRIGATION HEAD SYMBOLS							
RAIN BIRD NONPOTABLE PRODUCT, PURPLE CLIP-ON COVER FOR HEATS, PURPLE LID FOR VALVE BOX							
SYMBOL	NOZ.	MANF.	MODEL NO.	RAD.	PATTERN	PSI	FLOW GPM
TREE BUBBLERS							
⊙	-	RAINBIRD	1402	5'	-	30	.5
DRIP							
---	-	RAINBIRD	SURFACE DRIPLINE XFS-04-12-500	-	18" O.C.	30	.4 GPH

* NOTE: SHRUB SPRAY HEAD DIRECTLY AGAINST WALLS MAY USE 18" RISER INSTEAD OF 12" POP UP BODY.

NOTE:

- CONTRACTOR MAY SELECT IRRIGATION COMPONENTS FROM ALTERNATIVE MANUFACTURER WITH EQUAL PERFORMANCE.
- PROVIDE UF DIRECT BURIAL CONTROL WIRE WITH WATERPROOF CONNECTIONS.
- ALL IRRIGATION PIPE SHALL BE PURPLE PIPE WITH "RECYCLED WATER" LETTERS PRINTED ON THE PIPE
- ALL VALVE BOXES SHALL BE PURPLE WITH PURPLE "RECYCLED WATER" TAGS AND HEAT BRAND ALL BOX LIDS ACCORDINGLY.

1.04 SHRUB & G.C. L.

A-3 3/4"

FLOW/ GALLON PER MIN.
CONTROL VALVE STATION NUMBER
VALVE SIZE
HYDROZONE DESCRIPTION (L- LOW WATER USE, M.-MODERATE WATER USE, WST.-WARM SEASON TURF)

POC, CONTROLLER AND MAINLINE NOTES

POINT OF CONNECTION
MAKE IRRIGATION POINT OF CONNECTION INTO HOUSE SERVICE LINE DIRECTLY DOWNSTREAM OF THE WATER METER. DETERMINE FINAL LOCATION IN THE FIELD AND ADJUST AS NECESSARY. INSTALL GATE VALVE AT POC FOR IRRIGATION SYSTEM ISOLATION. EXTEND MAINLINE TO REMOTE CONTROL VALVES AS SHOWN. FINAL VALVE LOCATIONS TO BE APPROVED IN THE FIELD BY THE OWNER. INSTALL ALL VALVES PER LOCAL CODES. CONTRACTOR SHALL VERIFY STATIC PRESSURE PRIOR TO START OF INSTALLATION. IF A PRESSURE RELATED ISSUE IS IDENTIFIED, NOTIFY LANDSCAPE ARCHITECT IMMEDIATELY.

CONTROLLER
INSTALL IRRIGATION CONTROLLER AS SHOWN ON THE PLANS. FINAL CONTROLLER LOCATION TO BE APPROVED IN THE FIELD BY THE OWNER. THE OWNER IS TO PROVIDE 120V AC POWER TO THE FINAL CONTROLLER LOCATION. THE IRRIGATION CONTRACTOR SHALL MAKE ALL FINAL CONNECTIONS PER LOCAL CODES. MOUNT, GROUND, AND WIRE ALL THE CONTROL EQUIPMENT PER THE MANUFACTURER'S DIRECTIONS, THESE PLANS, AND PER ALL LOCAL CODES.

MAINLINE
MAINLINE AND VALVES SHOWN OUTSIDE OF PLANTED AREAS FOR CLARITY ONLY. INSTALL ALL IRRIGATION EQUIPMENT IN ADJACENT PLANTED AREAS EXCEPT WHERE SLEEVING IS SHOWN ON THE PLANS. ALL PIPES AND WIRES THAT MUST RUN UNDER HARDSCAPE TO BE SLEEVED IN SCH 40 PVC SLEEVES ACCORDING TO THE SLEEVING CHART BELOW.

SLEEVE	WIRES	PIPE
1.25" SLEEVE	1-8 WIRES	N/A
1.5" SLEEVE	9-16 WIRES	1/2" PIPE
2" SLEEVE	17-26 WIRES	3/4" PIPE
2.5" SLEEVE	27-38 WIRES	1" PIPE

WATER CONSERVATION CERTIFICATION STATEMENT:

"I HEREBY CERTIFY, AS THE LANDSCAPE ARCHITECT/ LICENSED LANDSCAPE PROFESSIONAL OF RECORD THAT THE INFORMATION PROVIDED HEREIN MEETS THE REQUIREMENTS AND STANDARDS AS OUTLINED IN THE MUNICIPAL CODE SECTION 19.7 AND THE ADOPTED DESIGN GUIDELINES FOR THE DEVELOPMENT OF THIS PROJECT."

LANDSCAPE ARCHITECT: JENNY YE
LICENSE: CA 5059

SIGNATURE: [Signature]
DATE: 02-28-22

RECLAIM WATER NOTES:

- THE INSTALLATION OF THE RECYCLED WATER SYSTEM SHALL CONFORM TO THE REGULATIONS FOR THE CONSTRUCTION OF RECYCLED WATER SYSTEMS WITHIN THE CITY OF ONTARIO.
- ALL ON-SITE RECYCLED AND POTABLE WATER PIPING INSTALLED ON PROJECT SHALL BE IDENTIFIED IN ACCORDANCE WITH CITY'S REGULATIONS AND IRRIGATION SPECIFICATIONS.
- ALL IRRIGATION EQUIPMENT (VALVE BOXES, IRRIGATION HEADS AND ROTORS, IRRIGATION VALVES, ETC.) SHALL HAVE PURPLE TAGS / CAPS TO IDENTIFY RECYCLED WATER IRRIGATION SYSTEM.
- ALL PRESSURE MAINLINE PIPING FROM THE RECYCLED WATER SYSTEM SHALL BE INSTALLED TO MAINTAIN A 10 FEET MINIMUM HORIZONTAL SEPARATION FROM ALL POTABLE WATER PIPING. WHERE RECYCLED AND POTABLE WATER PRESSURE MAINLINE PIPING CROSS, THE RECYCLED WATER PIPING SHALL BE INSTALLED BELOW THE POTABLE WATER PIPING IN A PVC CLASS 200 PIPE SLEEVE WHICH EXTENDS A MINIMUM OF 5 FEET ON EITHER SIDE OF THE POTABLE WATER PIPING.
- ALL VALVE COVERS FOR ON-SITE RECYCLED WATER SHALL BE PURPLE IN COLOR AND HEAT BRANDED WITH THE WORDS "RECYCLED WATER"
- ALL MAINLINE VALVES SHALL BE ACCESSIBLE DURING PROJECT DEVELOPMENT. ALL VALVE STEM TOPS HAVING OVER FOUR (4) FEET COVER ARE TO HAVE AN EXTENSION INSTALLED AND PINNED TO THE VALVE OPERATING NUT
- WARNING SIGNS AND LABELS SHOULD READ "THIS SITE IS IRRIGATED WITH RECLAIMED WATER", AND SHOULD BE IN BOTH ENGLISH AND IN SPANISH. THE SIGNS SHOULD INCLUDE THE INTERNATIONAL SYMBOL FOR DO NOT DRINK. MINIMUM TWO SIGNS PER METER.
- CONTRACTOR SHALL NOTIFY CITY TWO WORKING DAYS PRIOR TO THE START OF CONSTRUCTION.

OFF-SITE LANDSCAPE AREA

HYDROZONE	SQ. FT.
L LOW WATER USE PLANTING	2,699
TOTAL AREA:	2,699

REVISIONS	BY
CD 12-08-21	
02-28-22	

JYC
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Email: jennyhye@yahoo.com

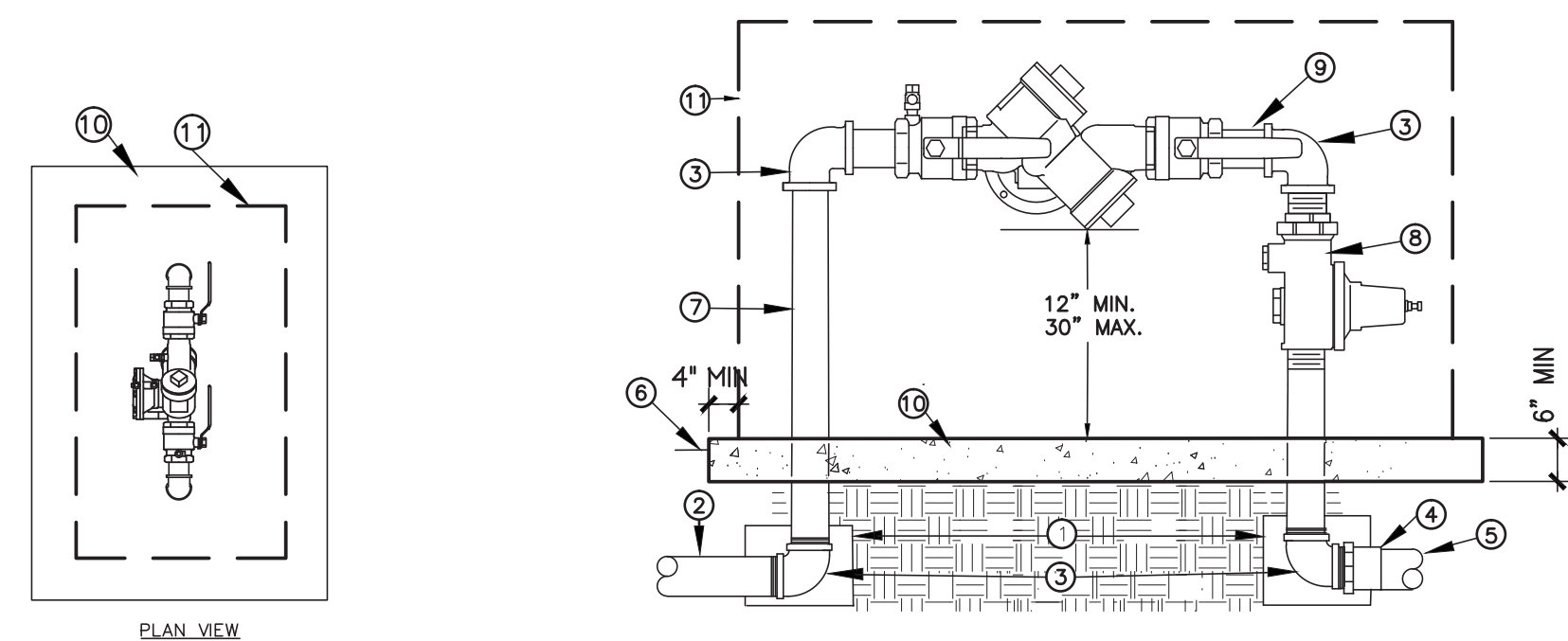


OFF-SITE IRRIGATION PLAN
FOR "CUP-21-05080"

SHEET TITLE:

PROJECT NAME:
CSLM CONSTRUCTION INC.
MAPES ROAD
PERRIS, CA 92570
APN 330-080-006

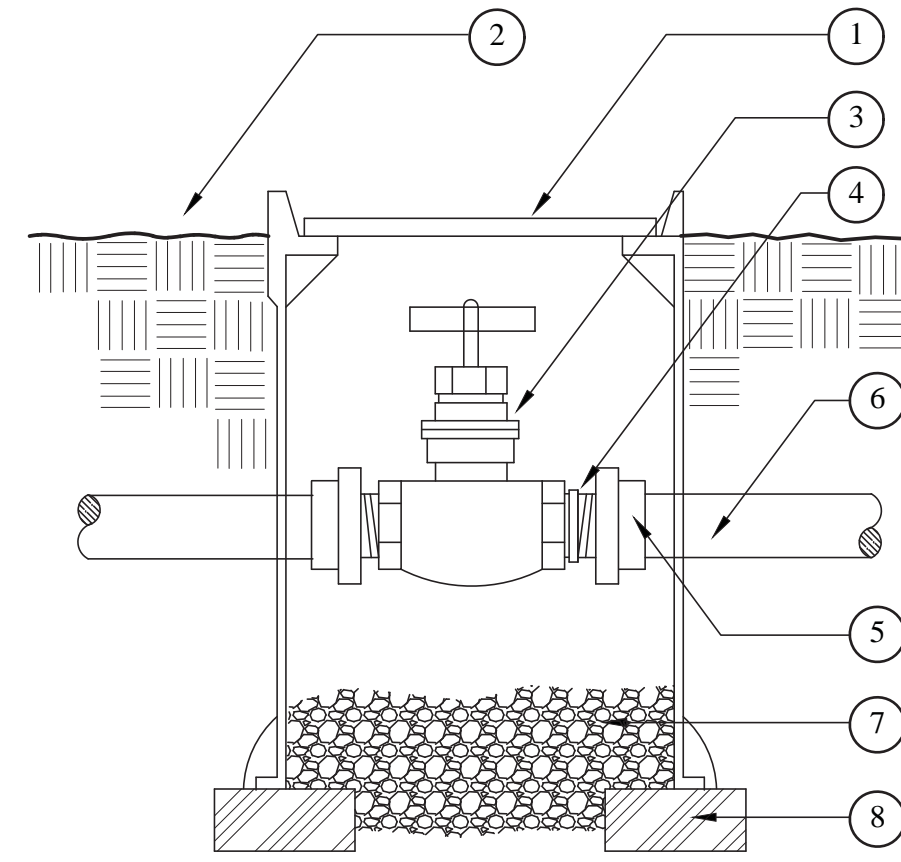
DATE	PER PLAN
02-28-22	
DRAWN	
JOB	21022
SHEET	



- 1 12"x12"x12" MINIMUM CONCRETE THRUST BLOCK
- 2 TO "POINT OF CONNECTION" - ADAPT AS NECESSARY
- 3 BRASS 90 DEGREE ELL - T&T
- 4 PVC SCH 40 MALE ADAPTER - SIZE AS REQUIRED
- 5 PVC MAINLINE PER LEGEND - SIZE PER PLAN
- 6 FINISH GRADE
- 7 BRASS NIPPLE - T&T - LENGTH AS REQUIRED
- 8 PRESSURE REGULATOR, PER LEGEND
- 9 BRASS NIPPLE - T&T - 4" MINIMUM LENGTH
- 10 CONCRETE PAD
- 11 ALUMINUM BACKFLOW ENCLOSURE WITH WEATHER RESISTANT INSULATION, SIZE & ANCHOR BOLTS AS PER ENCLOSURE SPECS.

NOTES: INSTALL BACKFLOW PREVENTER PER LOCAL CODES, HEALTH DEPARTMENT STANDARDS, AND MANUFACTURER'S DIRECTIONS. VERIFY LOCAL REQUIREMENTS PRIOR TO INSTALLATION. WHERE BACKFLOW DEVICE SIZE IS SMALLER THAN MAINLINE, ENTIRE APPARATUS TO BE SIZE OF MAINLINE. BUSH DOWN ONLY AT THE BACKFLOW DEVICE. FINAL LOCATION OF DEVICE TO BE DETERMINED IN THE FIELD. SCREEN WITH SHRUBS AS POSSIBLE.

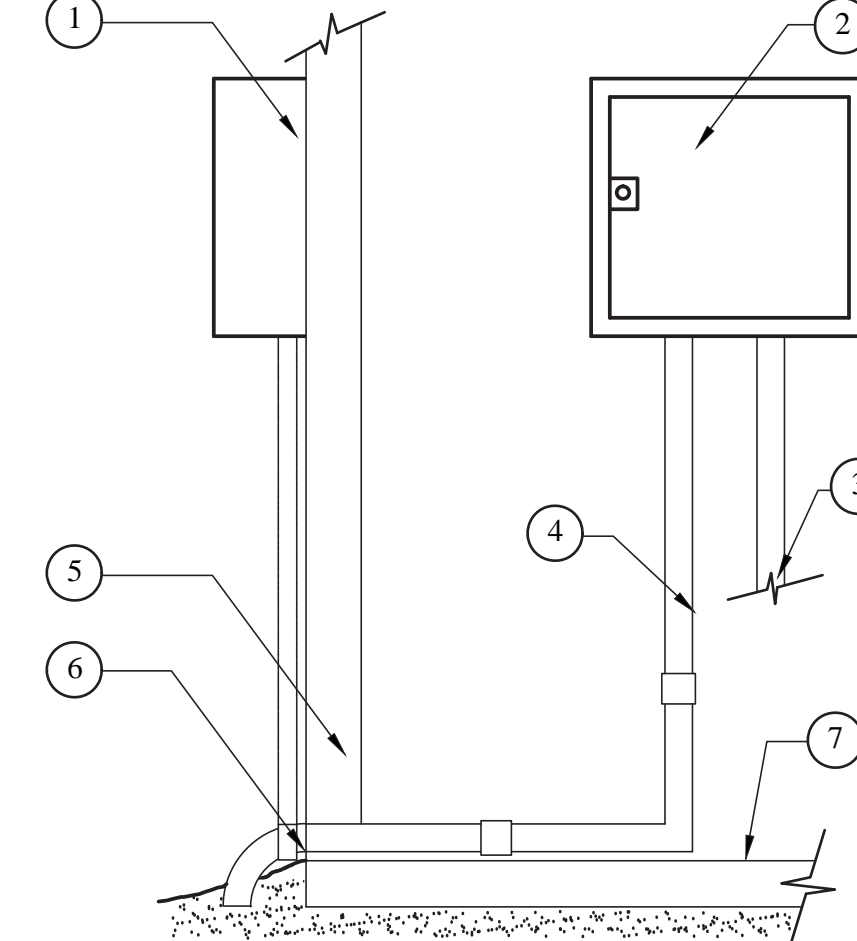
01 BACKFLOW AND PRESSURE REGULATOR 3/4"-1'-0"



- 1 APPROVED ROUND VALVE BOX WITH PURPLE LID W/ LOCKING TOP. HEAT BRAND "G.V." WITH 1/2" WIDE LETTERS.
- 2 FINISH GRADE
- 3 LINE SIZE GATE VALVE
- 4 PVC SCH. 80 NIPPLE
- 5 PVC ADAPTER
- 6 PVC MAINLINE SIZE AS NOTES ON PLAN
- 7 1 CU. PEA GRAVEL
- 8 2x4 REDWOOD BLOCK/ BRICK

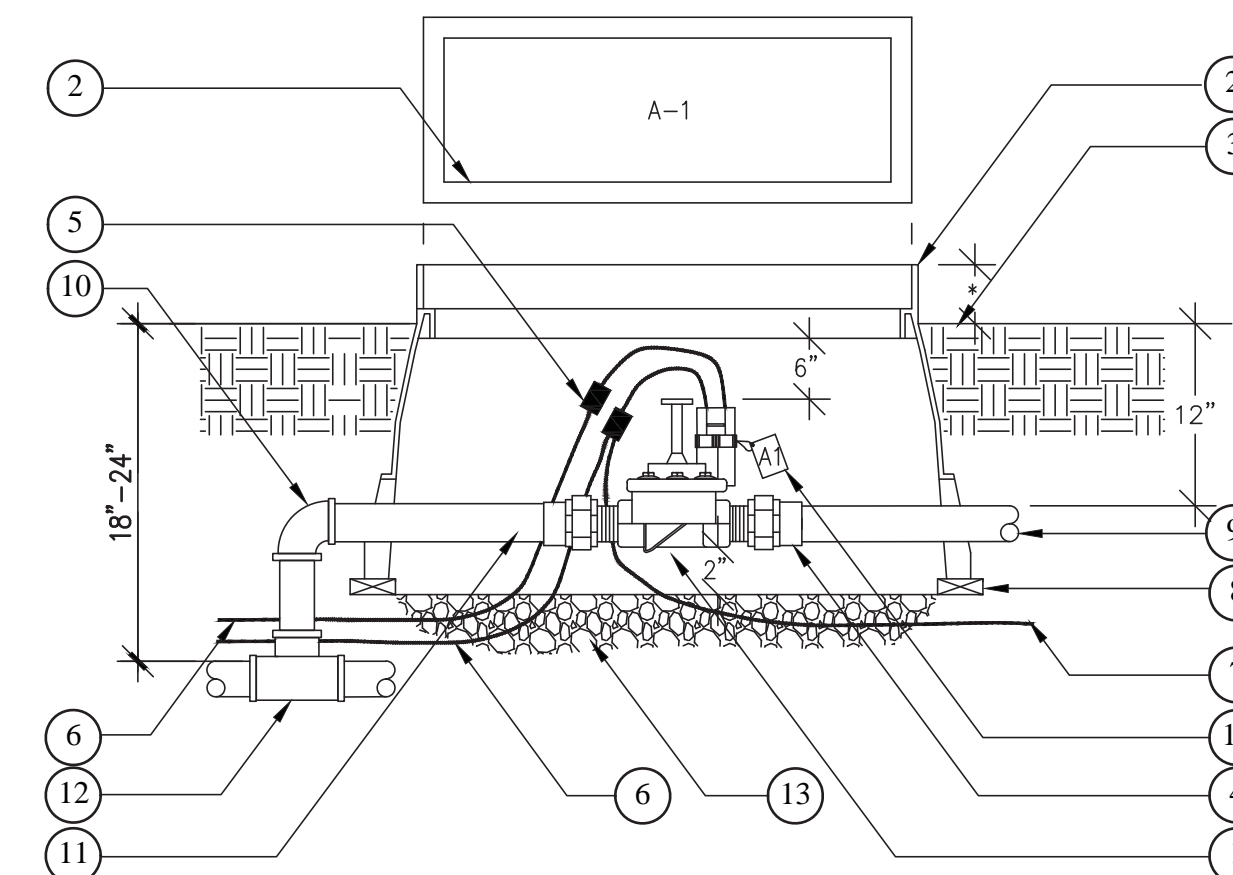
NOTE:
1. VALVES UP TO 2" DIA. SHALL HAVE MALLEABLE IRON TEE HANDLES; VALVES LARGER THAN 2" SHALL HAVE SQUARE OPERATING NUTS.
2. PURPLE PIPE SHALL BE USED THROUGHOUT THE PIPE NETWORK FOR RECLAIMED WATER.

02 GATE VALVE 3/4"-1'-0"



- 1 EXTERIOR MOUNTED CONTROLLER. SEE IRRIGATION PLAN FOR LOCATION.
- 2 INTERIOR MOUNTED CONTROLLER. SECURE TO WALL PER MANUFACTURER'S RECOMMENDATIONS.
- 3 120 VOLT WIRE IN CONDUIT
- 4 CONTROL WIRES IN PVC CONDUIT
- 5 WALL
- 6 SWEEP ELL
- 7 FLOOR

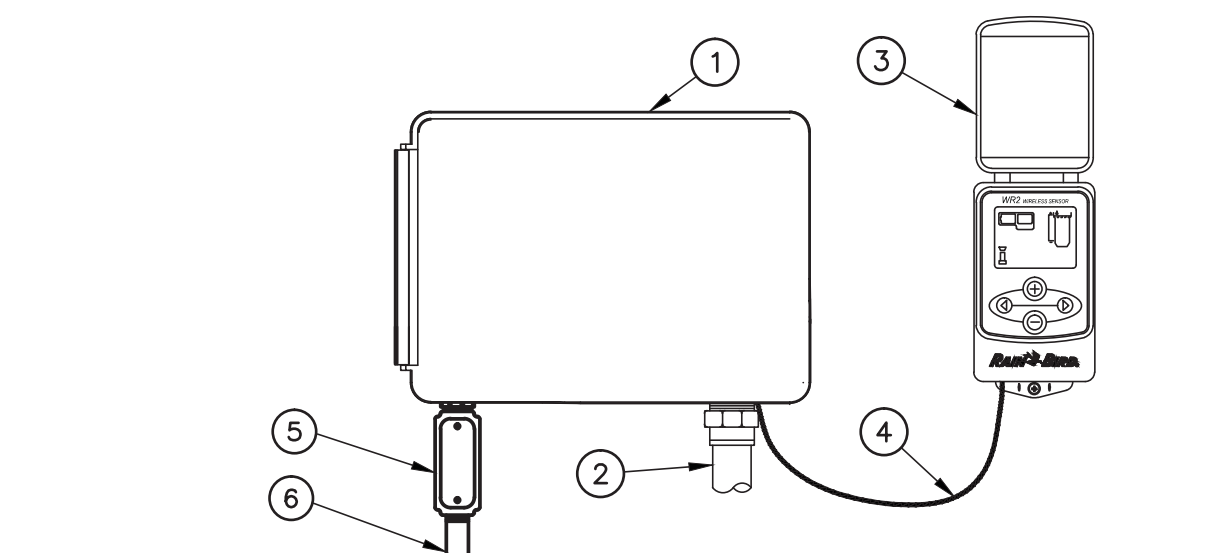
03 CONTROLLER 3/4"-1'-0"



- 1 ELECTRIC CONTROL VALVE PER LEGEND
- 2 12"x18" PLASTIC VALVE BOX WITH BOLT-DOWN PURPLE LID - HEAT BRAND STATION NUMBER ON LID IN 2" HIGH CHARACTERS
- 3 FINISH GRADE
- 4 PVC SCH 40 MALE ADAPTER - SIZE AS REQUIRED - TYP. OF 2
- 5 WATERPROOF WIRE CONNECTOR WITH 36" COILED EXPANSION LOOP
- 6 WIRES FROM CONTROLLER
- 7 COMMON WIRE TO OTHER VALVES ON SAME CONTROLLER
- 8 COMMON BRICK - 4 REQUIRED
- 9 PVC LATERAL LINE PER LEGEND
- 10 PVC SCH 40 ELL - SxS
- 11 PVC MAINLINE PER LEGEND
- 12 SCH 40 PVC MAINLINE FITTING
- 13 3/4" GRAVEL - 6" DEEP
- 14 PURPLE "CHRISTY'S" I.D. TAG OR APPROVED EQUAL

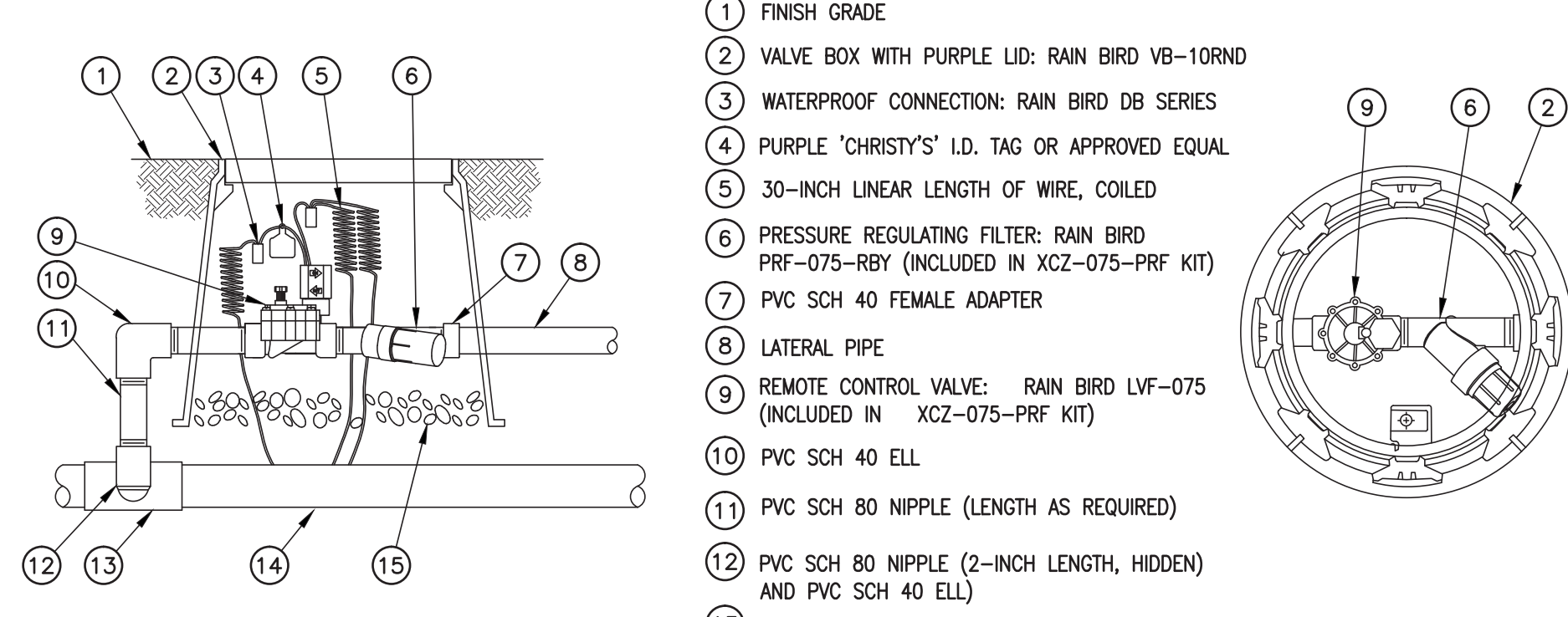
NOTES: TEE OFF MAINLINE TO BE SAME SIZE AS LATERAL LINE EXITING VALVE. ONLY REDUCE PIPE SIZE AT VALVE.
* 1/2" IN TURF, 2" IN PLANTER

06 CONTROL VALVE



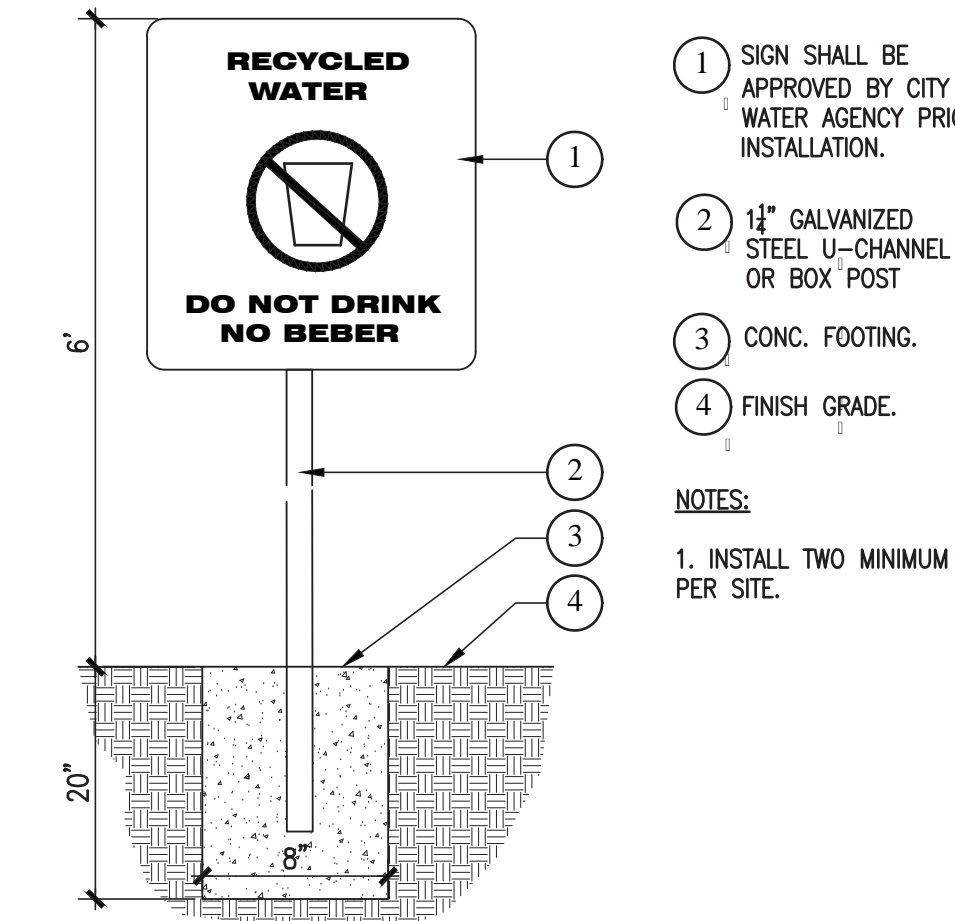
- 1 CONTROLLER:
- 2 CONDUIT AND FITTINGS FOR VALVE WIRES
- 3 WIRELESS SENSOR CONTROLLER INTERFACE
- 4 CABLE HARNESS FOR CONTROLLER INTERFACE
- 5 JUNCTION BOX
- 6 CONDUIT TO POWER SUPPLY

04 WIRELESS RAIN SENSOR



- 1 FINISH GRADE
- 2 VALVE BOX WITH PURPLE LID: RAIN BIRD VB-10RND
- 3 WATERPROOF CONNECTION: RAIN BIRD DB SERIES
- 4 PURPLE "CHRISTY'S" I.D. TAG OR APPROVED EQUAL
- 5 30-INCH LINEAR LENGTH OF WIRE, COILED
- 6 PRESSURE REGULATING FILTER: RAIN BIRD PRF-075-RBY (INCLUDED IN XCZ-075-PRF KIT)
- 7 PVC SCH 40 FEMALE ADAPTER
- 8 LATERAL PIPE
- 9 REMOTE CONTROL VALVE: RAIN BIRD LVF-075 (INCLUDED IN XCZ-075-PRF KIT)
- 10 PVC SCH 40 ELL
- 11 PVC SCH 80 NIPPLE (LENGTH AS REQUIRED)
- 12 PVC SCH 80 NIPPLE (2-INCH LENGTH, HIDDEN) AND PVC SCH 40 ELL
- 13 PVC SCH 40 TEE OR ELL
- 14 PVC MAINLINE, PURPLE FOR RECLAIMED WATER
- 15 3-INCH MINIMUM DEPTH OF 3/4-INCH WASHED GRAVEL

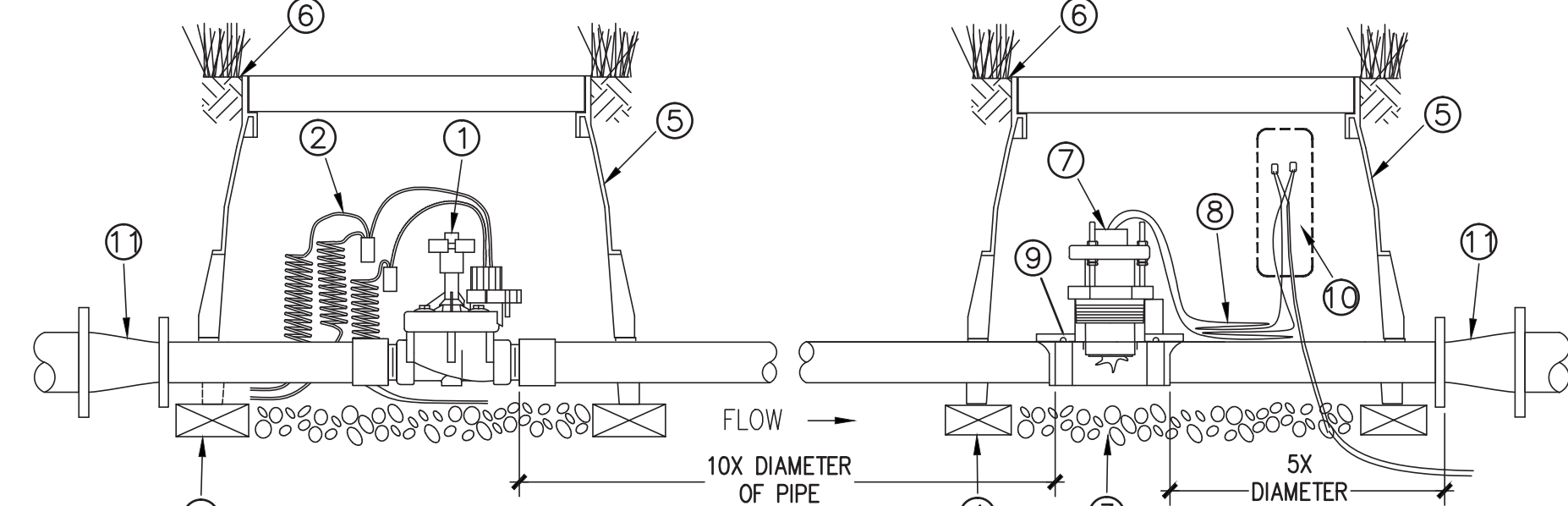
07 RAIN BIRD DRIP ZONE KIT



- 1 SIGN SHALL BE APPROVED BY CITY / WATER AGENCY PRIOR TO INSTALLATION.
 - 2 1 1/2" GALVANIZED STEEL U-CHANNEL OR BOX POST
 - 3 CONC. FOOTING.
 - 4 FINISH GRADE.
- NOTES:
1. INSTALL TWO MINIMUM PER SITE.

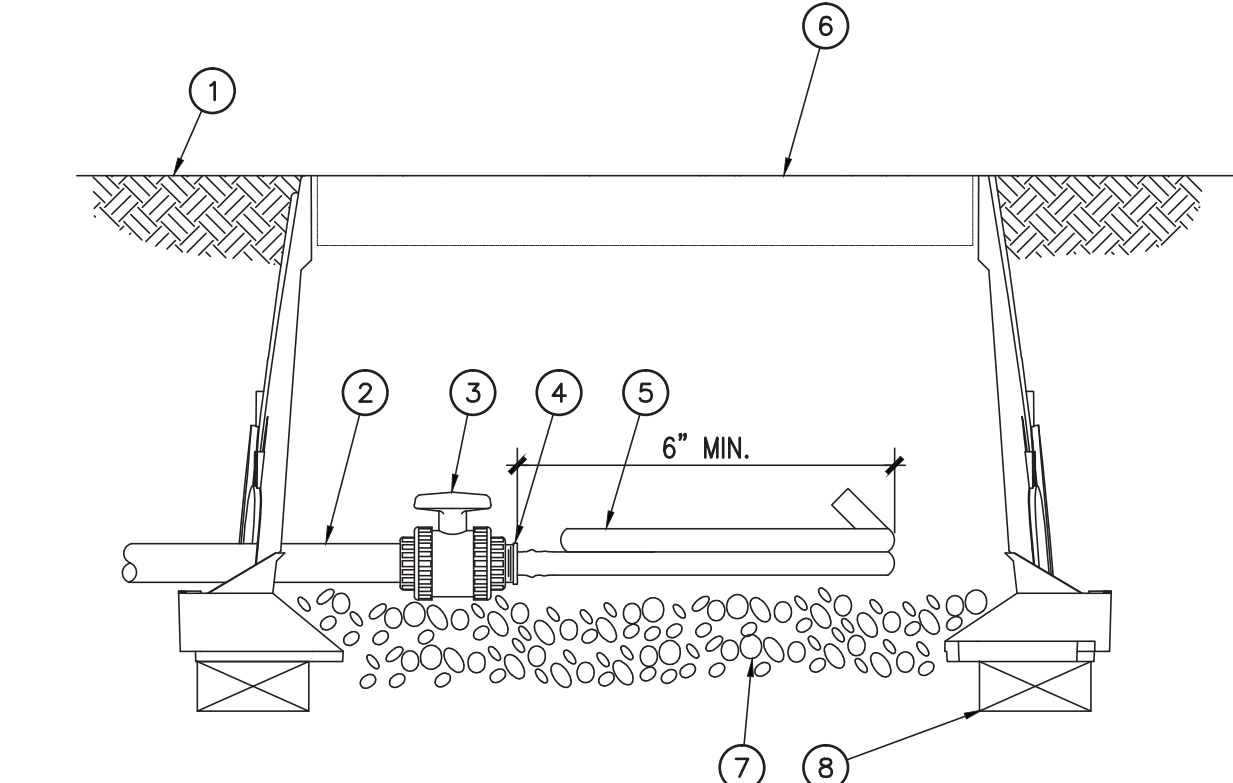
10 RECLAIMED WATER SIGN

05 TRENCHING SECTION



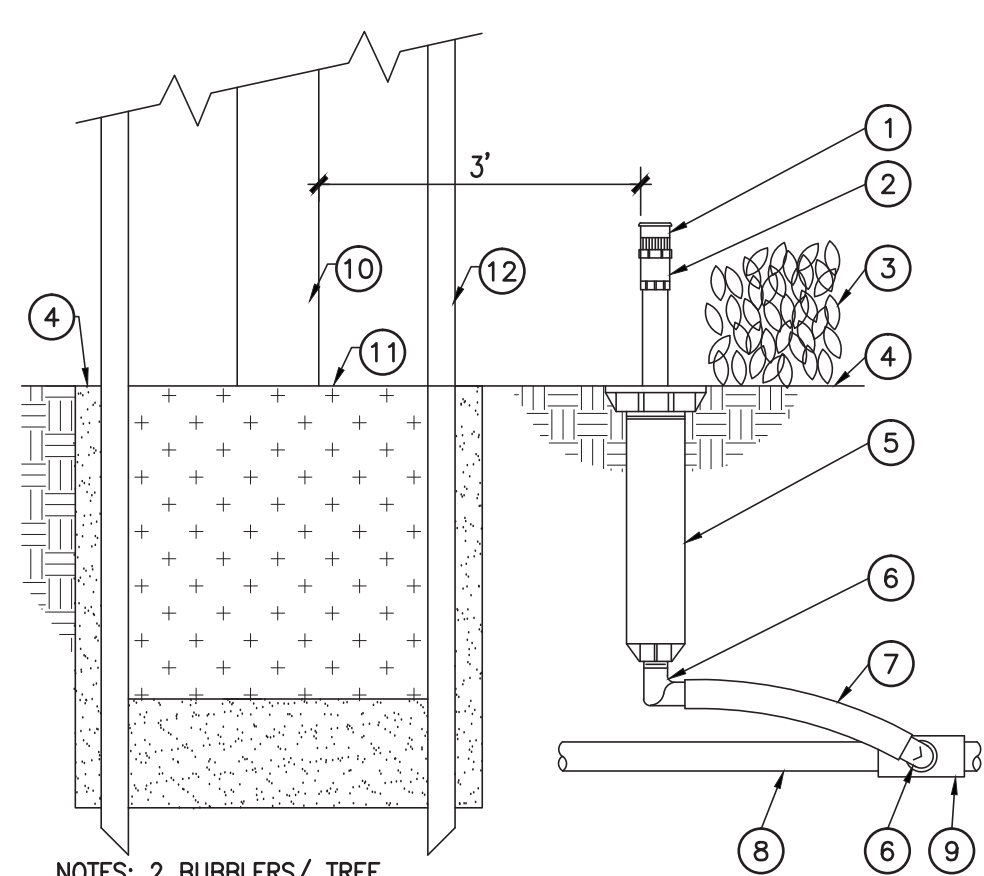
- 1 MASTER VALVE, REFER TO LEGEND
- 2 36-INCH LENGTH OF COILED WIRE TO SATELLITE CONTROLLER SPARE STATION
- 3 3-INCH MINIMUM DEPTH OF 3/4-INCH WASHED GRAVEL
- 4 BRICK (1 OF 4)
- 5 RECTANGULAR VALVE BOX WITH BOLT-DOWN PURPLE LID, W/ HEAT BRAND "MV" / "FS" ON LID IN 2" HIGH CHARACTERS.
- 6 FINISH GRADE
- 7 RAIN BIRD FS SERIES 350 FLOW SENSOR(S)
- 8 36-INCH LENGTH OF COILED PE-CABLE TO FLOW SENSING EQUIPMENT AT OCU/SATELLITE CONTROLLER ASSEMBLY
- 9 DOUBLE-STRAP SADDLE
- 10 SPLICE/ WIRING PER MANUFACTURER SPECIFICATION
- 11 CONCENTRIC REDUCER

08 MASTER VALVE AND FLOW SENSOR



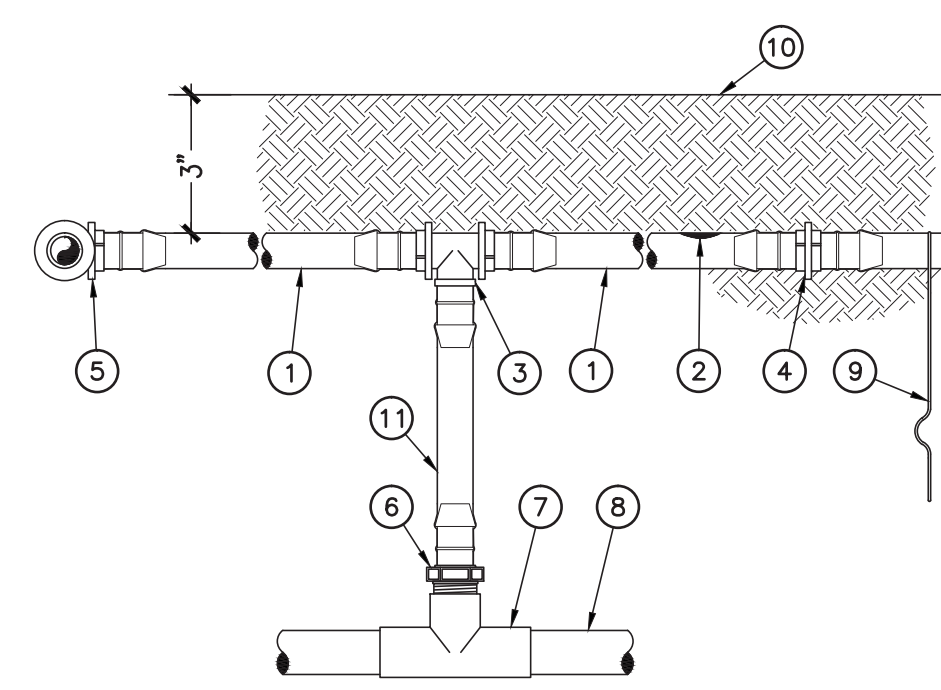
- 1 FINISH GRADE
- 2 PVC DRIP MANIFOLD PIPE
- 3 PVC 1" x 3/4" TRUE UNION BALL VALVE
- 4 EASY FIT MALE X BARB ADAPTER: RAIN BIRD XFF-MA-075
- 5 SUB-SURFACE DRILINE: RAIN BIRD XF SERIES BLANK TUBING
- 6 12-INCH VALVE BOX WITH COVER:
- 7 RAIN BIRD VB-STD 3-INCH MINIMUM DEPTH OF 3/4" WASHED GRAVEL
- 8 BRICK (1 OF 2)

09 MANUAL FLUSH VALVE



NOTES: 2 BUBBLERS/ TREE

11 TREE BUBBLER



- 1 RAIN BIRD XFS SUB-SURFACE DRILINE
- 2 INLINE DRIP EMITTER OUTLET, SEE PLANS FOR DRILINE OUTLET SPACING.
- 3 BARB TEE 17x17x17mm RAIN BIRD XFF-TEE
- 4 BARB COUPLING 17x17mm RAIN BIRD XFF-COUP
- 5 BARB ELBOW 17x17mm RAIN BIRD XFF-ELBOW
- 6 BARB MALE ADAPTER 17mm x 1/2" MPT RAIN BIRD XFF-MA-050 17mm x 3/4" MPT RAIN BIRD XFF-MA-075 PVC TEE SxSxT
- 7 PVC TEE SxSxT
- 8 PVC LATERAL SUPPLY HEADER
- 9 TIE DOWN STAKE: RAIN BIRD TDS-050 WITH BEND (TYPICAL)
- 10 FINISH GRADE
- 11 RAIN BIRD XF SERIES BLANK TUBING LENGTH AS REQUIRED

12 DRIP LINE

- 1 STREAM SPRAY BUBBLER ON POP-UPS SET 3' FROM TRUNK: RAIN BIRD 50B,
- 2 PLASTIC ADAPTER: RAIN BIRD MODEL PA-80
- 3 PLANT MATERIAL
- 4 FINISH GRADE/TOP OF MULCH
- 5 POP-UP SPRAY SPRINKLER: RAIN BIRD 1806
- 6 1/2-INCH MALE NPT x .490-INCH BARB ELBOW: RAIN BIRD MODEL SBE-050
- 7 SWING PIPE, 12-INCH LENGTH: RAIN BIRD MODEL SP-100
- 8 PVC LATERAL PIPE
- 9 PVC SCH 40 TEE OR ELL
- 10 TREE TRUNK
- 11 TREE ROOTBALL
- 12 2" LODGE POLE PINE STAKES
- 13 PLANT BACKFILL PER PLANTING PLAN

NOTES:
1. PLACE TIE DOWN STAKES EVERY THREE FEET IN SAND, FOUR FEET IN LOAM, AND FIVE FEET IN CLAY.
2. AT FITTINGS WHERE THERE IS A CHANGE OF DIRECTION SUCH AS TEES OR ELBOWS, USE TIE-DOWN STAKES ON EACH LEG OF THE CHANGE OF DIRECTION.
3. SAVE YOUR HANDS. USE THE RAIN BIRD FITTINGS-TOOL XF INSERTION TOOL FOR FITTING ASSEMBLY.

REVISIONS
CD 12-08-21
02-28-22

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LANDSCAPE ARCHITECTURE
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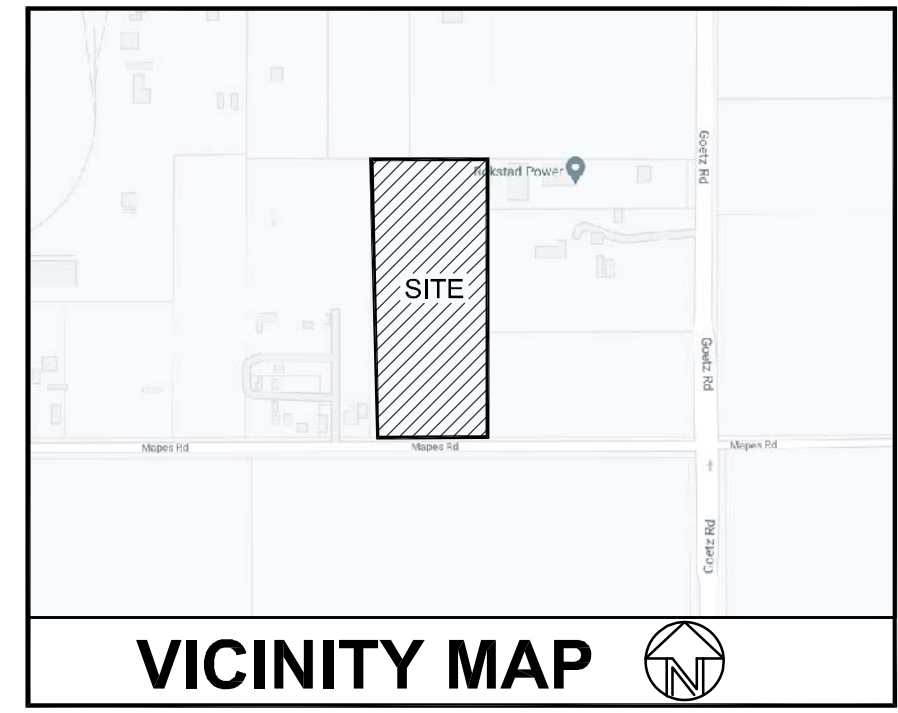
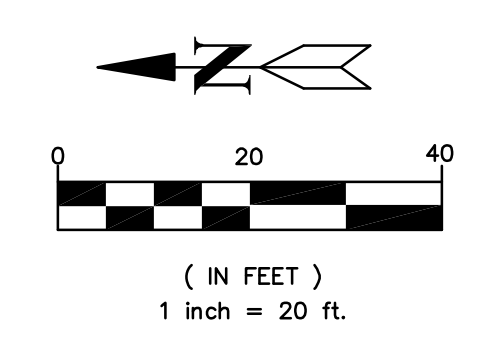
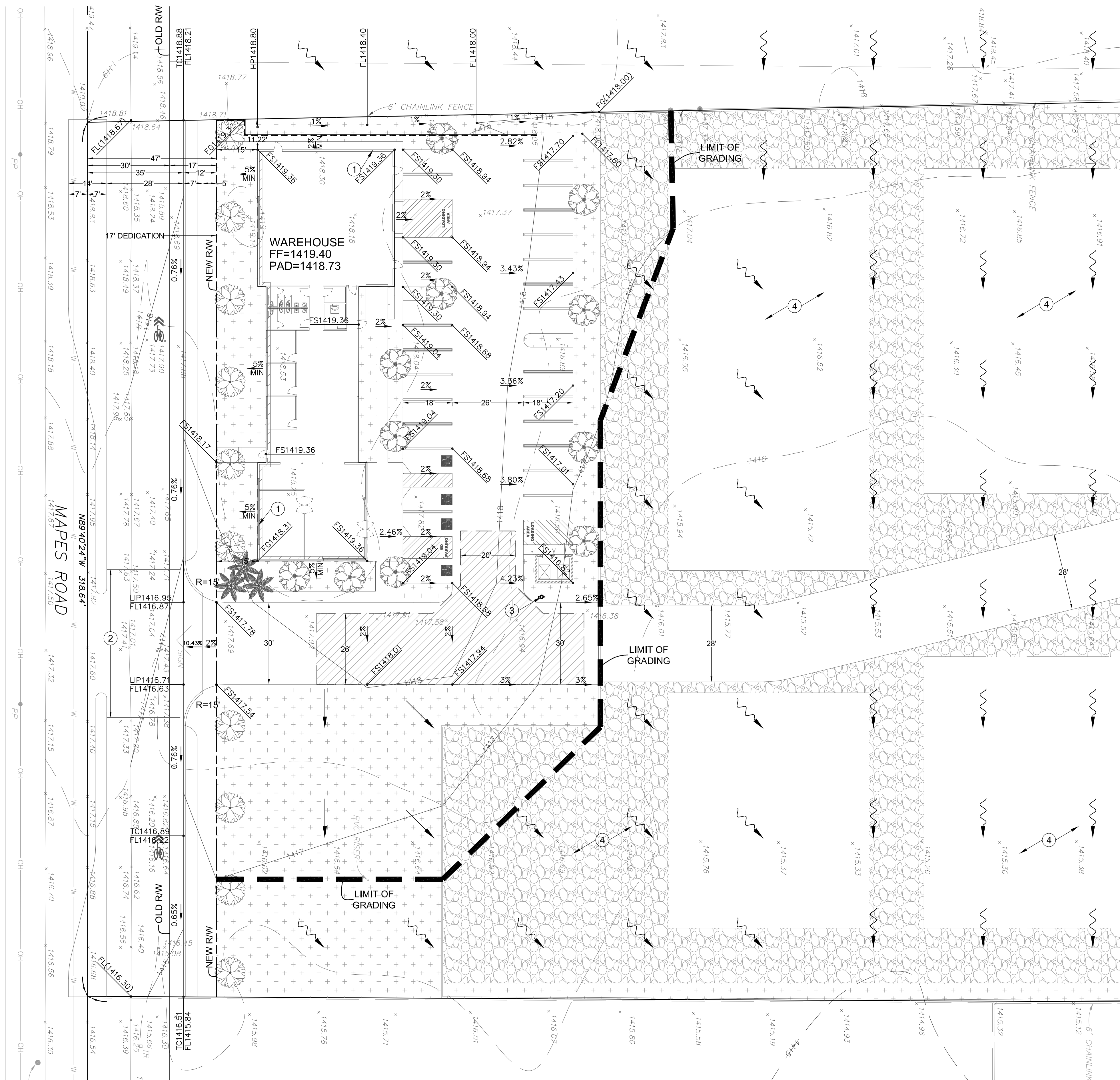
REGISTERED LANDSCAPE ARCHITECT
STATE OF CALIFORNIA

IRRIGATION DETAIL
FOR "CUP-21-05080"

CSLM CONSTRUCTION INC.
MAPES ROAD
PERRIS, CA 92570
APN 330-080-006

DATE 02-28-22
SCALE PER PLAN
DRAWN
JOB 211022
SHEET
L-4
OF 4 SHEETS

PRELIMINARY GRADING PLAN



LEGAL DESCRIPTION:

THAT PORTION OF THE SOUTHERLY 25 ACRES OF THE SOUTHEAST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 6, TOWNSHIP 5 SOUTH, RANGE 3 WEST, SAN BERNARDINO BASE AND MERIDIAN, IN THE CITY OF PERRIS, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, LYING EAST OF THE FOLLOWING DESCRIBED LAND.

BEGINNING AT A POINT IN THE SOUTH LINE OF SECTION 6K, DISTANT 379.3 FEET EASTERLY FROM THE SOUTHWEST CORNER OF THE ABOVE DESCRIBED SOUTHERLY 25 ACRES; THENCE NORTHERLY IN A STRAIGHT LINE TO A POINT IN THE NORTHERLY LINE OF SAID SOUTHERLY 25 ACRES DISTANT 357 FEET EAST FROM THE NORTHWEST CORNER OF SAID SOUTHERLY 25 ACRES.

EXCEPTING THEREFROM THE EASTERLY 622.9 FEET THEREOF.

BENCHMARK:

THE BENCHMARK FOR THIS SURVEY IS THE COUNTY OF RIVERSIDE STANDARD BRASS DISK STAMPED "600-31-68" LOCATED 2 MILES NORTH ON MURRIETA ROAD FROM CATHOLIC CHURCH IN SUN CITY, 700 FT NORTH OF INTERSECTION OF MURRIETA ROAD AND ETHANAC ROAD, 40 FT WEST OF MURRIETA ROAD LEVEL WITH THE ROAD, 3 FT NORTHEAST OF THE SOUTH EAST CORNER OF 8 FT CHAIN LINK FENCE OF E.M.W.D. MURRIETA PUMPING STATION, 2 FT NORTH OF MARKER POST, BRASS DISK IS SET IN THE TOP OF A CONCRETE POST;

ELEVATION = 1414.416, NGVD29 UPDATED 05/85.

EARTHWORK QUANTITY:

CUT: 68 CU.YD. EXPORT DIRT: 18 CU.YD.
 FILL: 86 CU.YD.

NOTE:
 THE YARDAGE SHOWN HEREON IS FOR PERMIT AND BONDING PURPOSES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING OF THE QUANTITIES.

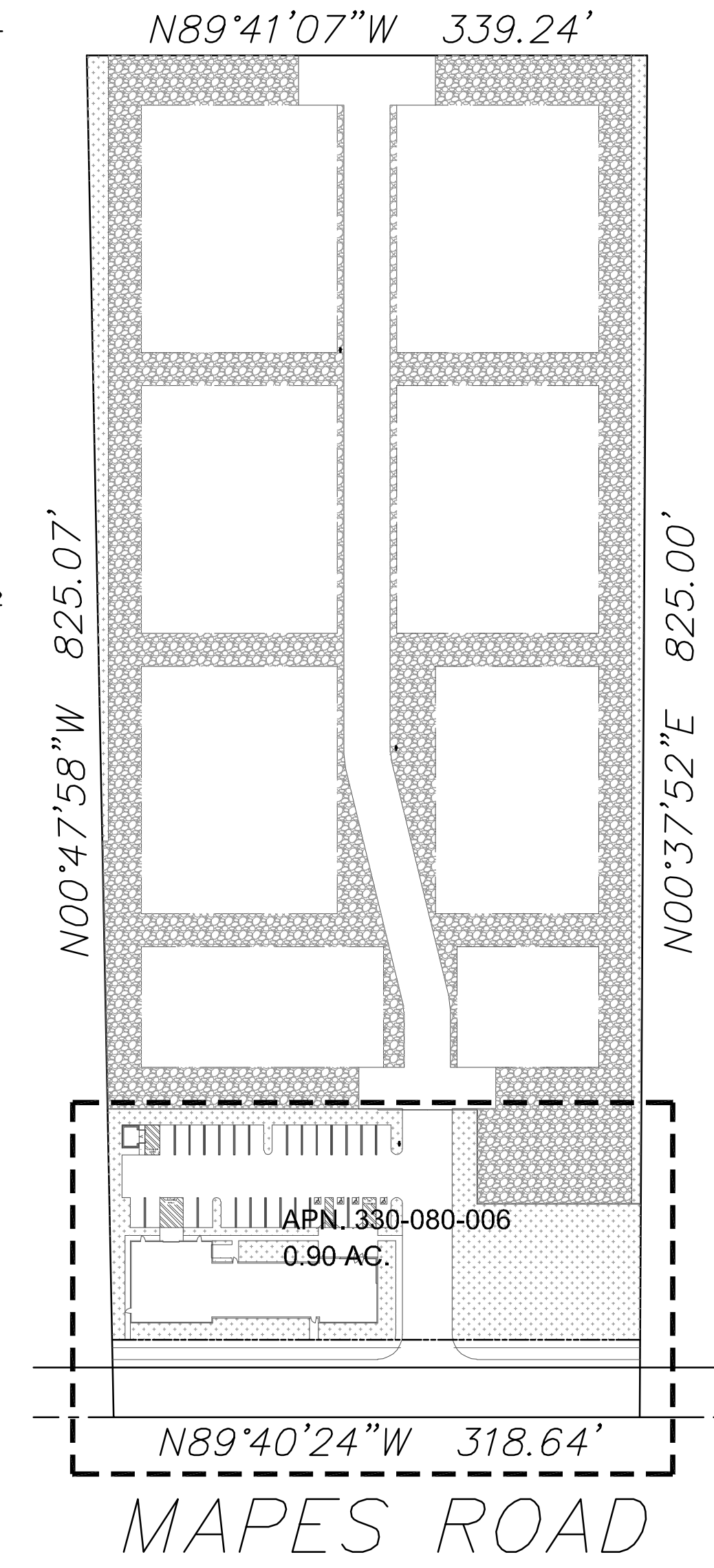
PROPOSED NOTES:

- ① PROPOSED NEW BUILDING PER ARCHITECTURAL PLAN
- ② PROPOSED NEW DRIVEWAY APPROACH
- ③ PROPOSED FIRE HYDRANT
- ④ INFILTRATION TRENCH

TOTAL AREA	261,792.10 SF	6.01 ACRES
TOTAL AREA AFTER DEDICATION	256,358.80 SF	5.88 ACRES
LIMIT OF GRADING	39,410.38 SF	0.90 ACRES
IMPERVIOUS AREA	41,253.56 SF	0.95 ACRES

SEE SHEET 2

- LEGEND:**
-LANDSCAPE AREA
 -INFILTRATION TRENCH
 -EXISTING DRAINAGE FLOW

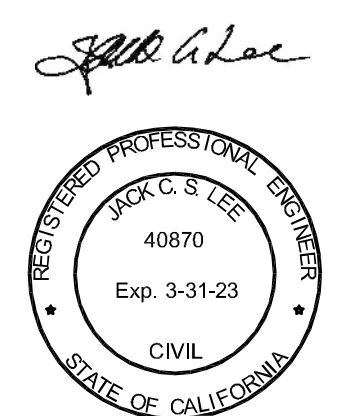


KEY MAP
 SCALE: 1" = 80'

CALLANDE ENGINEERING, INC.
 dba QUARTECH CONSULTANTS
 576 E. LAMBERT ROAD, BREAA, CA 92821
 TEL: (714) 671-1050 FAX: (714) 671-1090

PROJECT LOCATION:
 APN: 330-080-006
 MAPES ROAD,
 PERRIS, CA 92570

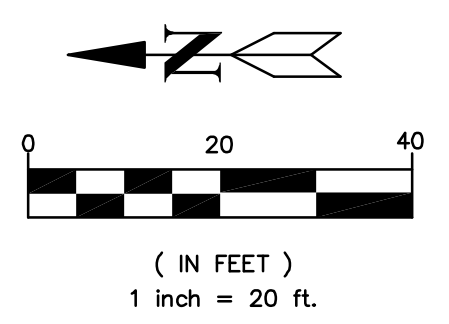
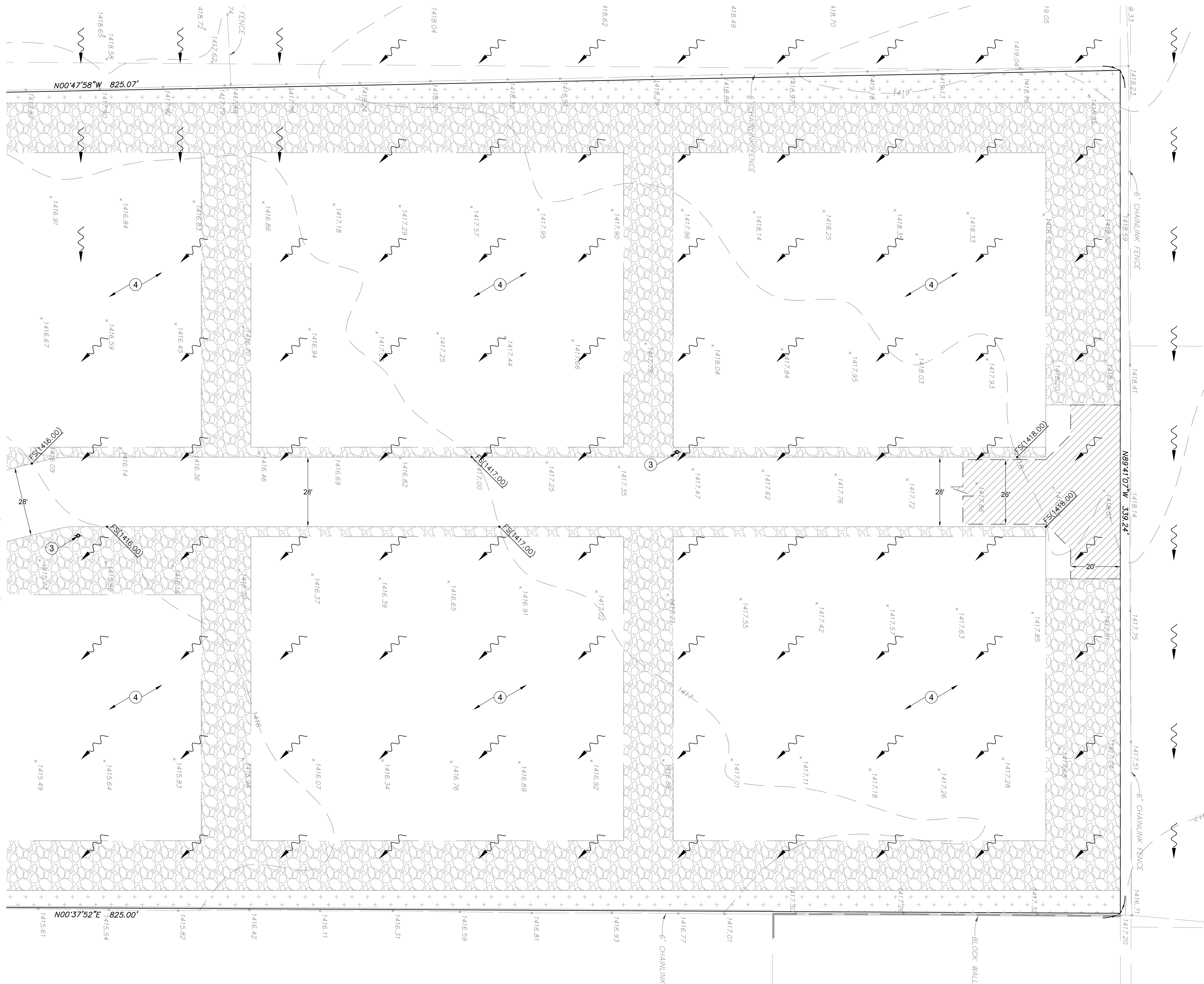
DRAWN: PS
 CHECKED: PS
 DATE: 04/25/2022
 JOB NO.: 21-188-001
 SCALE: 1" = 20'
 FILE NAME:



C-1

PRELIMINARY GRADING PLAN

SEE SHEET 1



- PROPOSED NOTES:**
- ③ PROPOSED FIRE HYDRANT
 - ④ INFILTRATION TRENCH

- LEGEND:**
-LANDSCAPE AREA
 -INFILTRATION TRENCH
 -EXISTING DRAINAGE FLOW

Jack C. Lee
 REGISTERED PROFESSIONAL ENGINEER
 JACK C. LEE
 40870
 Exp. 3-31-23
 CIVIL
 STATE OF CALIFORNIA

CALLAND ENGINEERING, INC.
 dba QUARTECH CONSULTANTS
 576 E. LAMBERT ROAD, BREA, CA 92821
 TEL: (714) 671-1050 FAX: (714) 671-1090

PROJECT LOCATION:
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 PERRIS, CA 92570

DRAWN: PS
 CHECKED:
 DATE: 04/25/2022
 JOB NO: 21-188-001
 SCALE: 1" = 20'
 FILE NAME:

C-2
 SHEET 2 OF 2

PAGE BREAK





RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

June 14, 2022

Alex Perez, Project Planner
City of Palm Springs Planning Department
3200 East Tahquitz Canyon Way
Palm Springs, CA 92262

CHAIR
Steve Manos
Lake Elsinore

VICE CHAIR
Russell Betts
Desert Hot Springs

COMMISSIONERS
Vacant

John Lyon
Riverside

Steven Stewart
Palm Springs

Richard Stewart
Moreno Valley

Michael Geller
Riverside

STAFF

Director
Paul Rull

Simon A. Housman
Jackie Vega
Barbara Santos

County Administrative Center
4080 Lemon St., 14th Floor
Riverside, CA 92501
(951) 955-5132

www.rcaluc.org

RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW – DIRECTOR’S DETERMINATION

File No.: ZAP1100PS22
Related File No.: 3.4321 MAJ (Major Architectural Review)
APN: 669-430-016 & 669-430-019

Dear Mr. Perez:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to ALUC’s general delegation as per Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed City of Palm Springs case No. 3.4321 MAJ (Major Architectural Review), a proposal to construct a 127,200 square foot self-storage building on 6.43 acres, located at 410 W. San Rafael Drive.

The site is located within Airport Compatibility Zone E of the Palm Springs International Airport Influence Area (AIA). Within Compatibility Zone E of the Palm Springs International Airport Land Use Compatibility Plan, non-residential intensity is not restricted.

The elevation of Runway 8-26 at its northerly terminus is 474.4 feet above mean sea level (AMSL). At a distance of approximately 12,066 feet from the runway to the site, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with top of roof exceeding 595.4 feet AMSL. The site’s finished floor elevation is 624 feet AMSL and the proposed building height is 38 feet, for a top point elevation of 662 feet AMSL. Therefore, FAA Obstruction Evaluation Service review for height/elevation was required. Therefore, review of the buildings for height/elevation reasons by the FAA Obstruction Evaluation Service (FAAOES) is required. The applicant has submitted Form 7460-1, and FAA OES has assigned Aeronautical Study No. 2022-AWP-11003-OE to this project. A Determination of No Hazard to Air Navigation letter was issued (Aeronautical Study No. 2022-AWP-11003-OE), as the FAA OES determined that the project would not result in an impact to air navigation. The FAA OES conditions have been incorporated into ALUC’s conditions listed below.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2005 Palm Springs International Airport Land Use Compatibility Plan, provided that the City of Palm Springs applies the following recommended conditions:

CONDITIONS:

1. Any new outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:

- (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, outdoor production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Any use which results in a hazard to flight, including physical (e.g. tall objects), visual, and electronic forms of interference with the safety of aircraft operations.
3. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property.
 4. The Federal Aviation Administration has conducted an aeronautical study of the proposed project (Aeronautical Study No. 2022-AWP-11003-OE) and has determined that neither marking nor lighting of the structure(s) is necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 M and shall be maintained in accordance therewith for the life of the project.
 5. The proposed structures shall not exceed a height of 38 feet above ground level and a maximum elevation at top point of 624 feet above mean sea level.
 6. The maximum height and top point elevation specified above shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in structure height or elevation shall not require further review by the Airport Land Use Commission. The specific coordinates, frequencies, and power shall not be amended without further review by the Federal Aviation Administration.
 7. Temporary construction equipment used during actual construction of the structure(s) shall not exceed 38 feet in height and a maximum elevation of 624 feet above mean sea level, unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.
 8. Within five (5) days after construction of the structure reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.) This requirement is also

applicable in the event the project is abandoned or a decision is made not to construct the applicable structure.

If you have any questions, please contact me at (951) 955-6893.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A handwritten signature in blue ink that reads "Paul Rull". The signature is written in a cursive style with a long horizontal stroke at the end.

Paul Rull, ALUC Director

Attachment: Notice of Airport in Vicinity
Aeronautical Study Number

cc: InSite Property Group (applicant/representative/property owner)
Harry Barrett, Airport Manager, Palm Springs International Airport
ALUC Case File

X:\AIRPORT CASE FILES\Palm Springs\ZAP1100PS22\ZAP1100PS22.LTR.doc



Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2022-AWP-11003-OE

Issued Date: 06/13/2022

Mike Diacos
InSite Property Group
19191 S. Vermont Avenue Suite 680
Torrance, CA 90502

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Commercial Use Building SecureSpace Self-Storage
Location:	Palm Springs, CA
Latitude:	33-51-35.42N NAD 83
Longitude:	116-33-00.05W
Heights:	624 feet site elevation (SE) 38 feet above ground level (AGL) 662 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 12/13/2023 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (206) 231-2877, or Nicholas.Sanders@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2022-AWP-11003-OE.

Signature Control No: 532735362-536952873

Nicholas Sanders
Technician

(DNE)

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

THERE IS AN AIRPORT NEARBY.

THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS

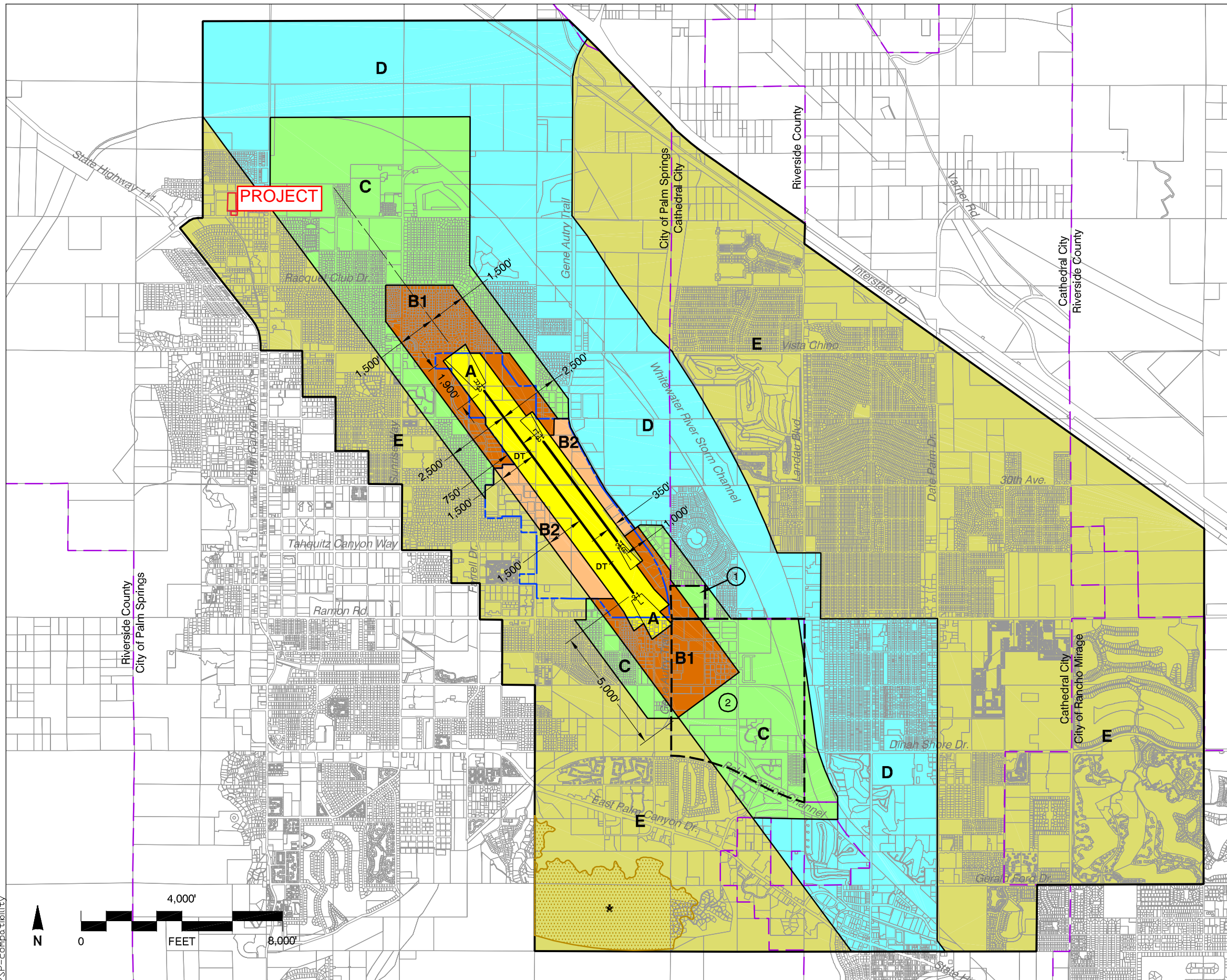
PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



Legend

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E
- Height Review Overlay Zone

Boundary Lines

- Airport Property Line
- City Limits

Notes

All dimensions measured from runway ends and centerlines.

DT = Displaced Threshold

See Chapter 2, Table 2A for compatibility criteria associated with this map.

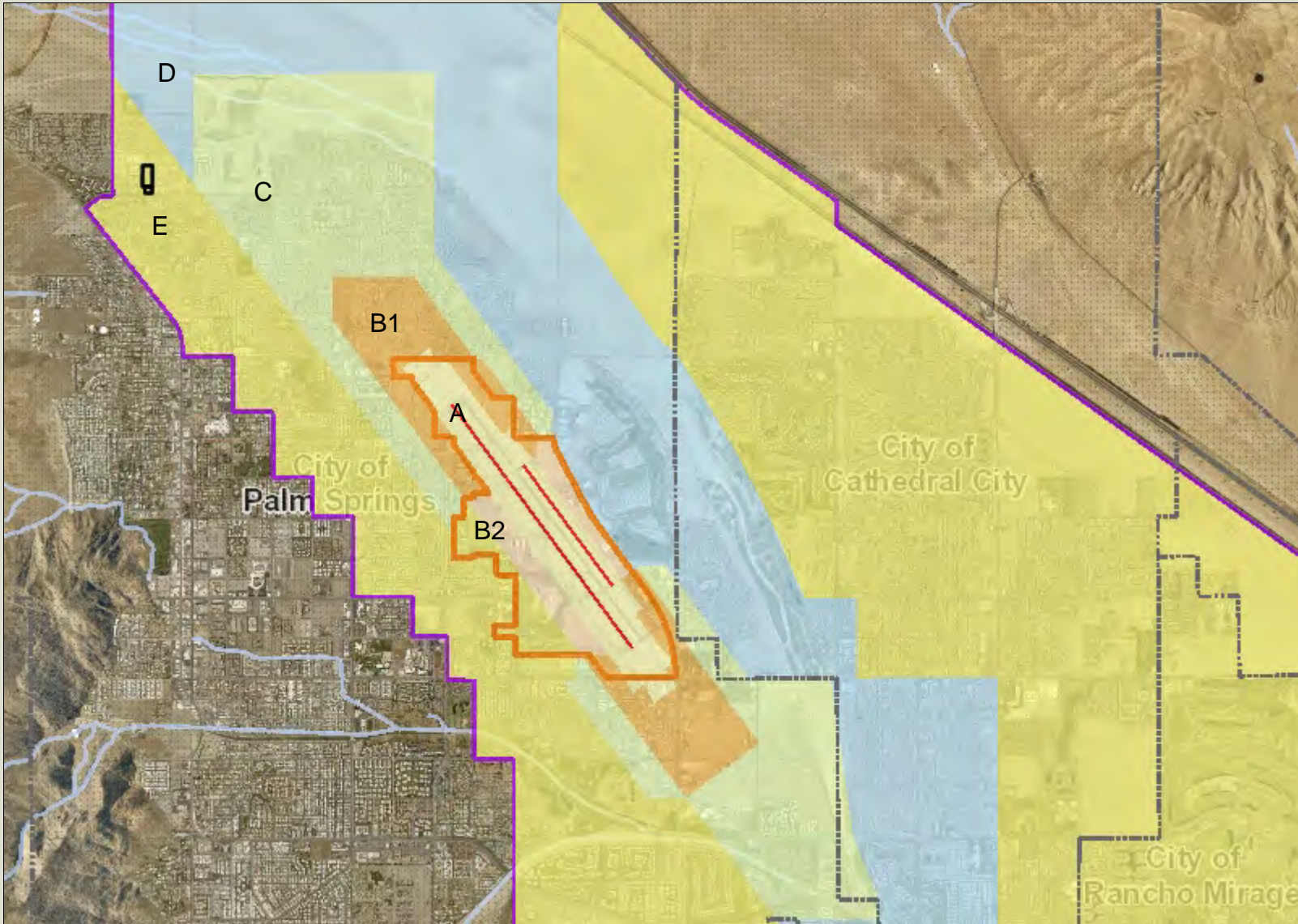
See Policy PS.2.1.

Riverside County
 Airport Land Use Commission
**Riverside County
 Airport Land Use Compatibility Plan
 Policy Document**
 (Adopted March 2005)

Map PS-1

Compatibility Map
 Palm Springs International Airport

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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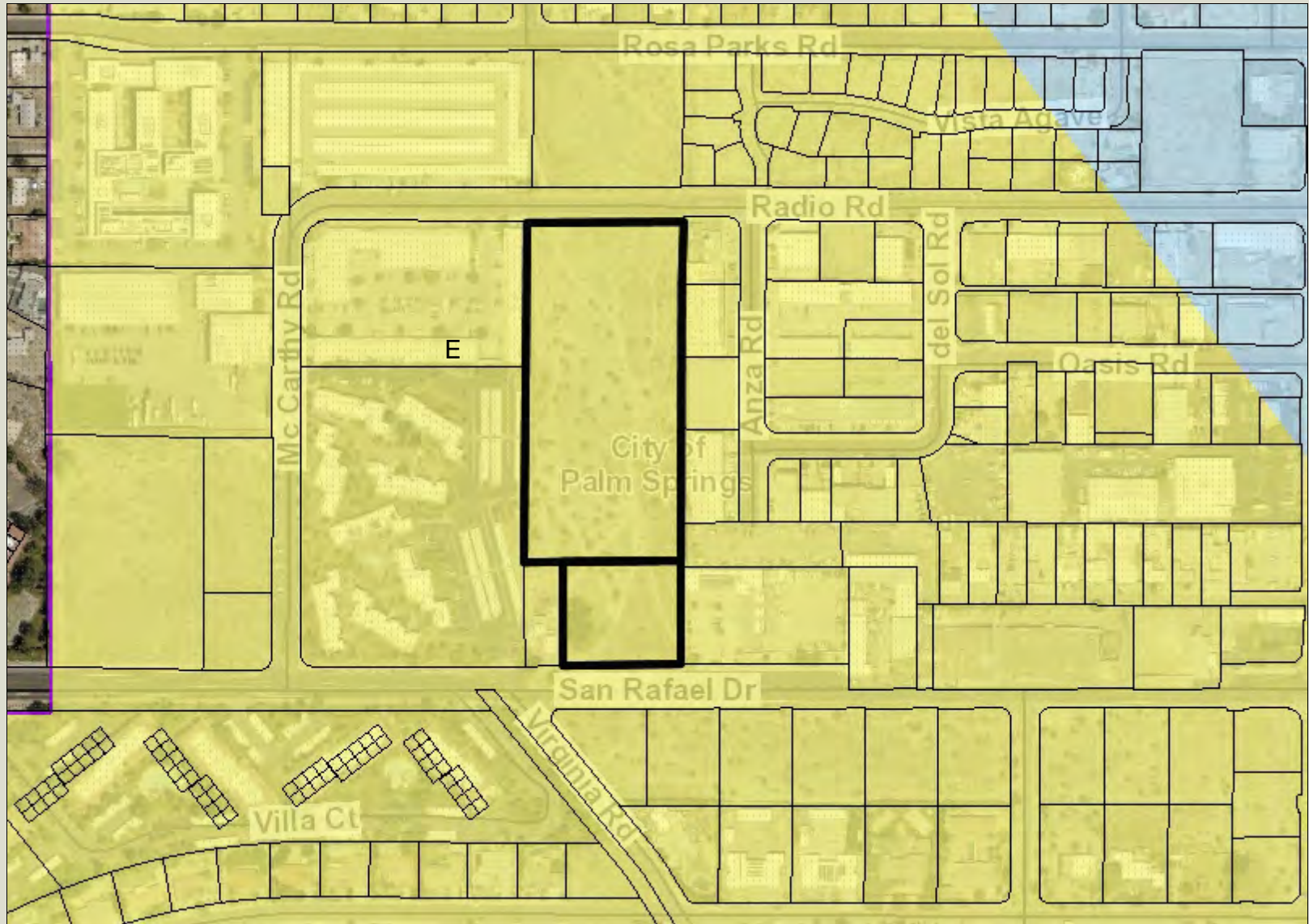


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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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Notes

Map My County Map



Legend

-  Parcels
-  County Centerline Names
-  County Centerlines
-  Blueline Streams
-  City Areas
-  World Street Map

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0 385 770 Feet

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Notes

Map My County Map



Legend

- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



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Notes



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Map My County Map



Legend

- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map

Notes



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Map My County Map



Legend

-  Blueline Streams
-  City Areas
-  World Street Map



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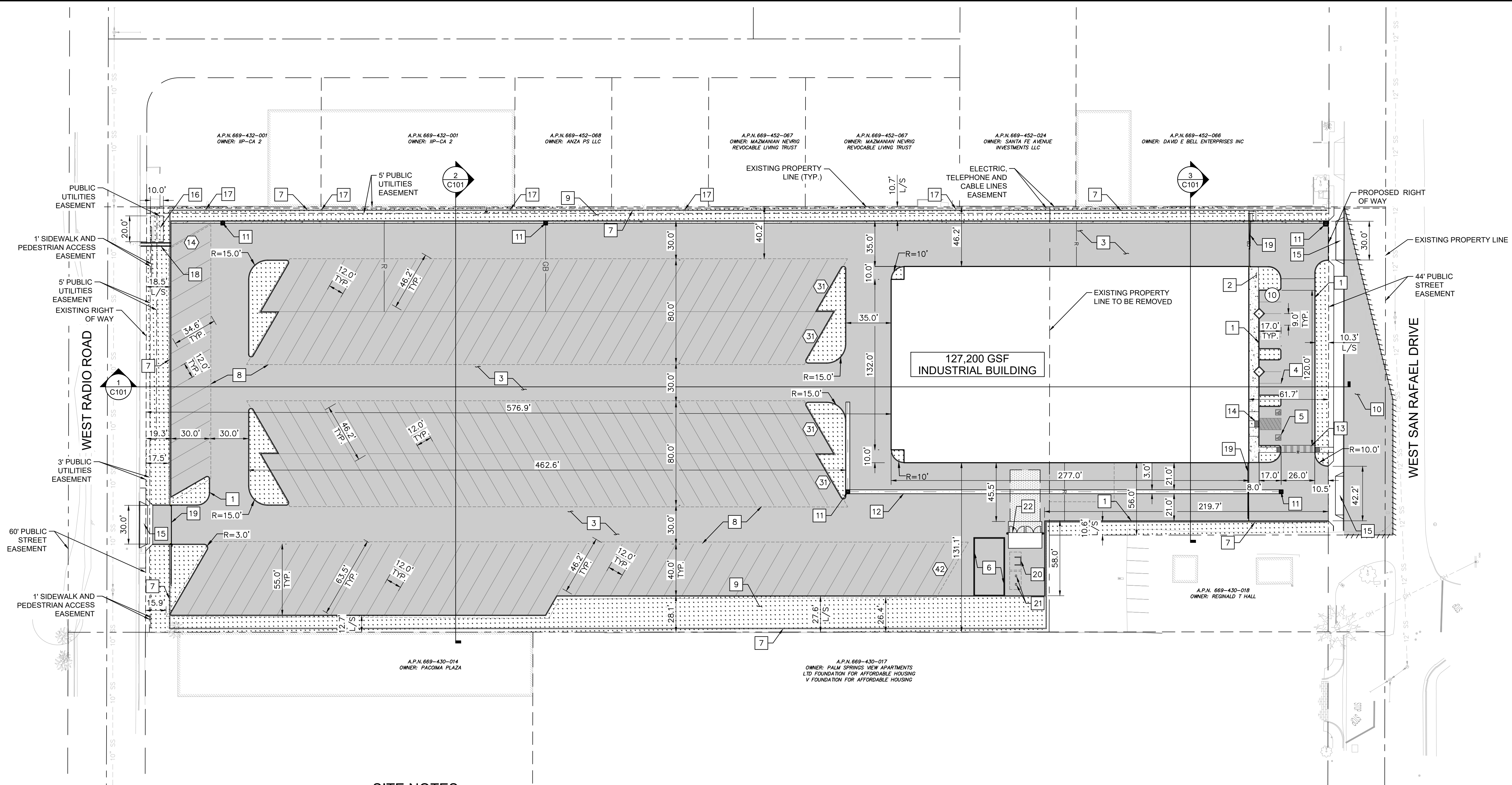
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Notes

K:\BAY_LDEV\197425015 - Insite - Palm Springs - BZA\03_CADD\PlanSheets\Entitlement_Packages\C100 - CIVIL SITE PLAN.dwg
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LEGEND

- PROPERTY LINE
- EASEMENT OR SETBACK LINE
- EXISTING LOT LINE
- EXISTING CHAIN LINK FENCE
- CENTER LINE
- SHADE STRUCTURE OVERHANG
- SAWCUT LINE
- CONCRETE VALLEY GUTTER
- CONCRETE SIDEWALK
- ASPHALT CONCRETE PAVEMENT
- LANDSCAPE/PLANTER AREA
- PARKING STALL COUNTER
- RV PARKING STALL COUNTER

SITE NOTES

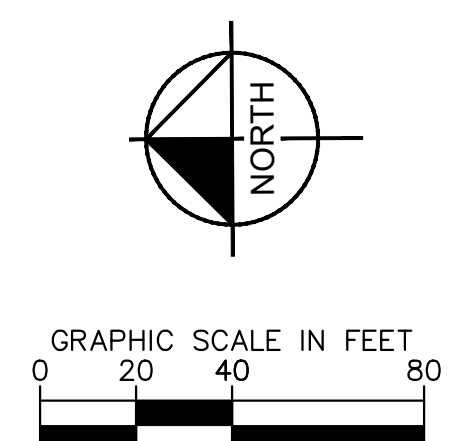
- 1 PROPOSED CURB.
- 2 PROPOSED CONCRETE SIDEWALK.
- 3 PROPOSED ASPHALT CONCRETE.
- 4 PROPOSED COMPACT 90° PARKING STALL.
- 5 PROPOSED ACCESSIBLE PARKING STRIPING.
- 6 PROPOSED RV WASH STATION.
- 7 PROPOSED 8" CMU WALL.
- 8 PROPOSED SHADE CANOPY OVER RV PARKING.
- 9 PROPOSED LANDSCAPING.
- 10 PROPOSED OFFSITE IMPROVEMENTS ON WEST SAN RAFAEL DRIVE.
- 11 PROPOSED STORM DRAIN INLET.
- 12 PROPOSED VALLEY GUTTER.
- 13 PROPOSED CROSSWALK STRIPING.
- 14 PROPOSED ACCESSIBLE RAMP.
- 15 INSTALL DRIVEWAY PER CITY STANDARD.
- 16 EXISTING TRANSFORMER TO REMAIN.
- 17 EXISTING POWERPOLE TO REMAIN.
- 18 PROPOSED OVERLAND RELEASE PATH OF TRAVEL TO PUBLIC RIGHT OF WAY.
- 19 PROPOSED SECURITY GATE.
- 20 PROPOSED DUMP STATION.
- 21 PROPOSED AIR/WATER UNIT.
- 22 PROPOSED TRASH ENCLOSURE.

NOTES

1. ALL CURBS ARE 2' IN RADIUS UNLESS DIMENSIONED OTHERWISE.
2. ALL DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.

PARKING SUMMARY TABLE	
STANDARD PARKING STALLS PROVIDED	8
ADA PARKING STALLS PROVIDED	2
COVERED RV PARKING	180
TOTAL PARKING STALLS PROVIDED	190

PROJECT INFORMATION	
PROJECT ADDRESS:	W SAN RAFAEL DR PALM SPRINGS, CA 92262
PARCEL NUMBER(S):	669430019 (5.23 ACRES) 669430016 (1.26 ACRES)
ZONING:	M-1
TOTAL SITE AREA:	6.43 ACRES
TOTAL DISTURBED AREA:	6.43 ACRES
FAR:	.45 (.5 ALLOWED)
SITE COVERAGE:	47% (60% ALLOWED)
BUILDING INFORMATION	
CONSTRUCTION TYPE:	TYPE II-B FULLY SPRINKLERED PER NFPA 13
OCCUPANCY:	S-1 STORAGE INDUSTRIAL
TOTAL BUILDING AREA:	127,200 SF
SETBACKS:	
FRONT:	25'-0"
SIDE:	10'-0" AND 25'-0" (RESIDENTIAL ADJACENT)
REAR:	10'-0"
PARKING REQUIRED:	6 STALLS
PARKING PROVIDED:	11 STALLS
CONTACT / APPLICANT	
INSITE PROPERTY GROUP DEVELOPMENT MANAGER MIKE DIACOS MIKE@INSITEPG.COM 805.766.0292	



NO.	REVISIONS	DATE	BY

Kimley-Horn

© 2022 KIMLEY-HORN AND ASSOCIATES, INC.
 4637 CHABOT DRIVE, SUITE 300, PLEASANTON, CA 94588
 PHONE: 925-398-4840 FAX: 925-398-4849
 WWW.KIMLEY-HORN.COM

PRELIMINARY
 FOR REVIEW ONLY NOT FOR
 CONSTRUCTION
 Kimley-Horn
 ENGINEER: JENNIFER ZIEBEL
 P.E. No. 48122
 Date: 05/05/22

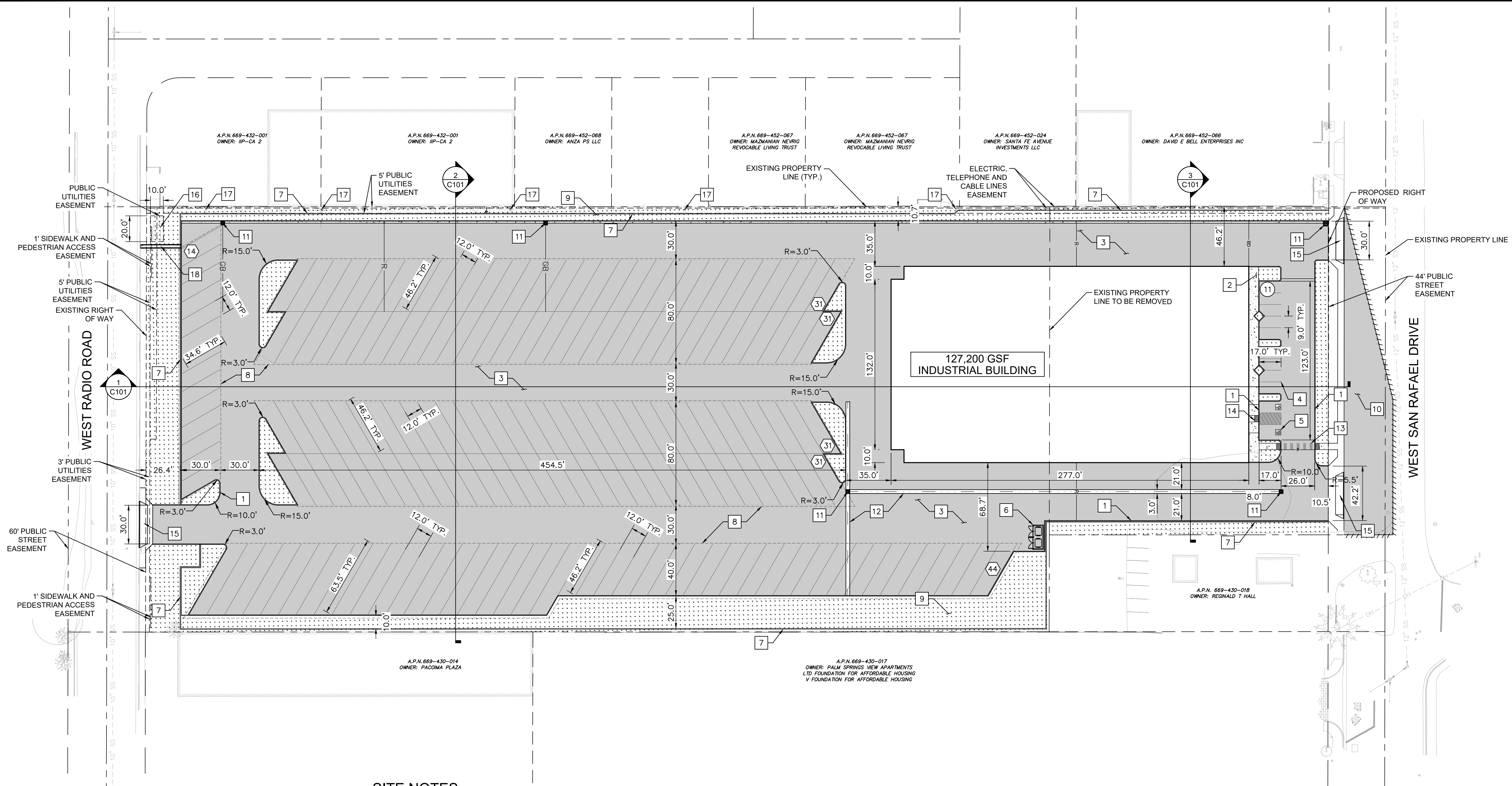
KHA PROJECT	197425015
DATE	5/5/22
SCALE	AS SHOWN
DESIGNED BY	BH
DRAWN BY	TC
CHECKED BY	BH

PRELIMINARY CIVIL
 SITE PLAN

SECURE SPACE SELF
 STORAGE
 PREPARED FOR
 INSITE PROPERTY GROUP
 CITY OF PALM SPRINGS CA

SHEET NUMBER
C100

K:\BAY_LDEV\197425015 - Insite - Palm Springs - BZA\03_CADD\PlanSheets\Entitlement_Packages\C100 - CIVIL SITE PLAN.dwg
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- EASEMENT OR SETBACK LINE
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- CENTER LINE
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- RV PARKING STALL COUNTER

SITE NOTES

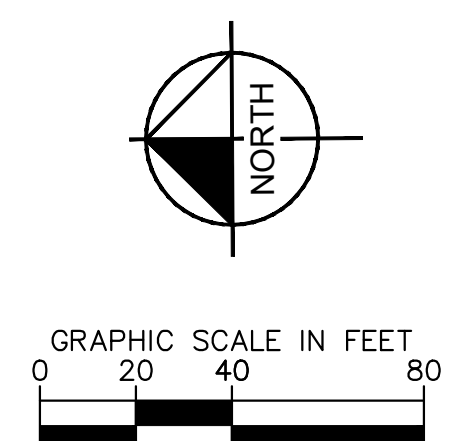
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- 3 PROPOSED ASPHALT CONCRETE
- 4 PROPOSED COMPACT 90° PARKING STALL
- 5 PROPOSED ACCESSIBLE PARKING STRIPING
- 6 PROPOSED TRASH ENCLOSURE
- 7 PROPOSED 8" CMU WALL
- 8 PROPOSED SHADE CANOPY OVER RV PARKING
- 9 PROPOSED LANDSCAPING
- 10 PROPOSED OFFSITE IMPROVEMENTS ON WEST SAN RAFAEL DRIVE
- 11 PROPOSED STORM DRAIN INLET
- 12 PROPOSED VALLEY GUTTER
- 13 PROPOSED CROSSWALK STRIPING
- 14 PROPOSED ACCESSIBLE RAMP
- 15 INSTALL DRIVEWAY PER CITY STANDARD
- 16 EXISTING TRANSFORMER TO REMAIN
- 17 EXISTING POWERPOLE TO REMAIN
- 18 PROPOSED OVERLAND RELEASE PATH OF TRAVEL TO PUBLIC RIGHT OF WAY

NOTES

1. ALL CURBS ARE 2' IN RADIUS UNLESS DIMENSIONED OTHERWISE.
2. ALL DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.

PARKING SUMMARY TABLE	
STANDARD PARKING STALLS PROVIDED	9
ADA PARKING STALLS PROVIDED	2
COVERED RV PARKING	182
TOTAL PARKING STALLS PROVIDED	193

PROJECT INFORMATION	
PROJECT ADDRESS:	W SAN RAFAEL DR PALM SPRINGS, CA 92262
PARCEL NUMBER(S):	669430019 (5.23 ACRES) 669430016 (1.26 ACRES)
ZONING:	M-1
TOTAL SITE AREA:	6.43 ACRES
TOTAL DISTURBED AREA:	6.43 ACRES
FAR:	.45 (.5 ALLOWED)
SITE COVERAGE:	47% (60% ALLOWED)
BUILDING INFORMATION	
CONSTRUCTION TYPE:	TYPE II-B FULLY SPRINKLERED PER NFPA 13
OCCUPANCY:	S-1 STORAGE INDUSTRIAL
TOTAL BUILDING AREA:	127,200 SF
SETBACKS:	
FRONT:	25'-0"
SIDE:	10'-0" AND 25'-0" (RESIDENTIAL ADJACENT)
REAR:	10'-0"
PARKING REQUIRED:	6 STALLS
PARKING PROVIDED:	11 STALLS
CONTACT / APPLICANT	
INSITE PROPERTY GROUP DEVELOPMENT MANAGER MIKE DIACOS MIKE@INSITEPG.COM 805.766.0292	



NO.	REVISIONS	DATE	BY

Kimley-Horn

© 2022 KIMLEY-HORN AND ASSOCIATES, INC.
 4637 CHABOT DRIVE, SUITE 300, PLEASANTON, CA 94568
 PHONE: 925-398-4840 FAX: 925-398-4849
 WWW.KIMLEY-HORN.COM

PRELIMINARY
 CONSULTATION NOT FOR
 CONSTRUCTION
 ENGINEER
Kimley-Horn
 ENGINEER: JENNIFER ZIEBEL
 PROJECT NO.: 197425015
 DATE: 03/17/22

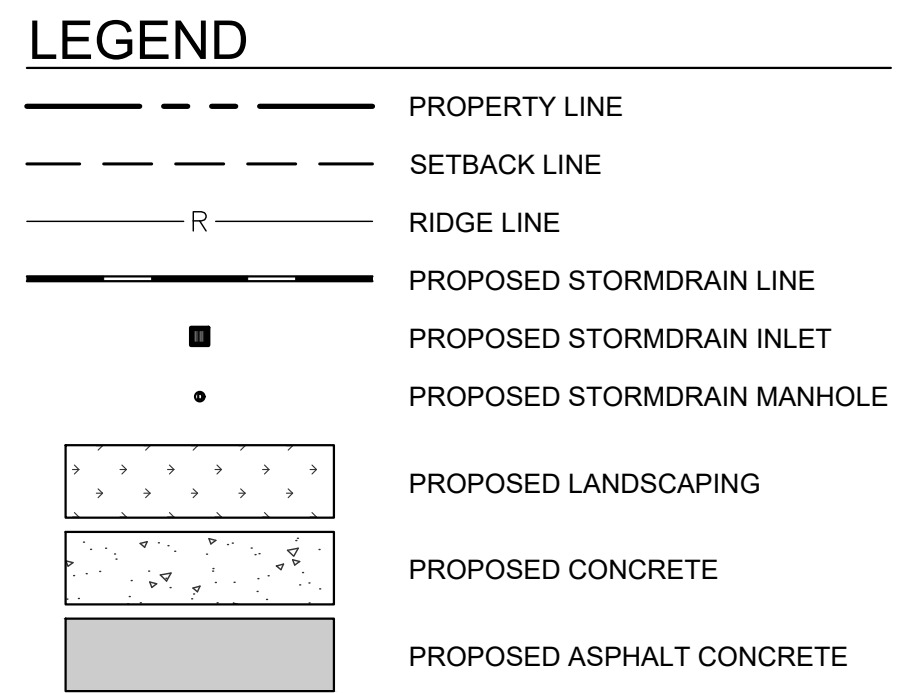
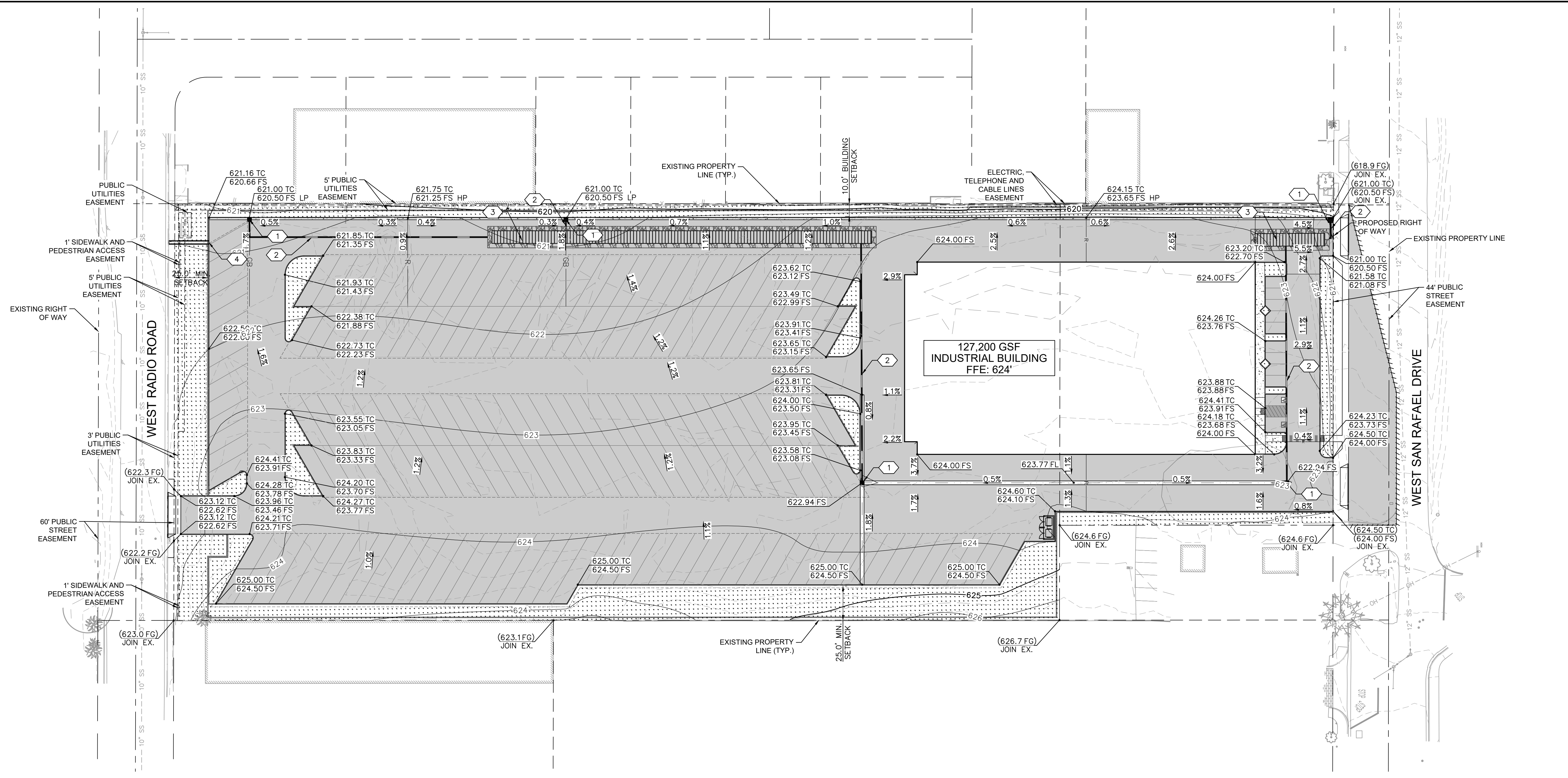
KHA PROJECT	197425015
DATE	3/17/22
SCALE	AS SHOWN
DESIGNED BY	BH
DRAWN BY	TC
CHECKED BY	BH

PRELIMINARY CIVIL
 SITE PLAN

SECURE SPACE SELF
 STORAGE
 PREPARED FOR
 INSITE PROPERTY GROUP
 CITY OF PALM SPRINGS CA

SHEET NUMBER
C100

K:\BAY_LDEV\197425015 - Insite - Palm Springs - BZA\03_CADD\PlanSheets\Entitlement_Packages\2200 - PRELIMINARY GRADING AND DRAINAGE PLAN.dwg
 This document, together with the concepts and designs presented herein, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.



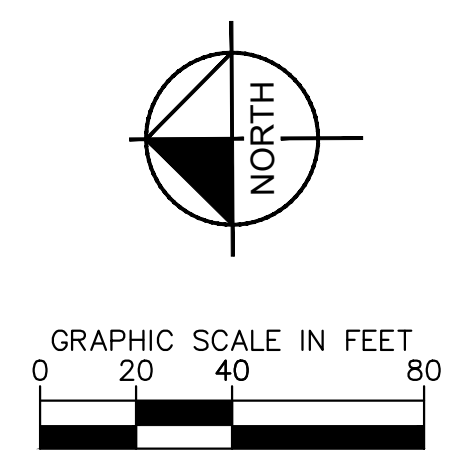
- GRADING AND DRAINAGE NOTES**
- ① PROPOSED STORM DRAIN INLET
 - ② PROPOSED STORM DRAIN PIPE
 - ③ PROPOSED UNDERGROUND CONTECH INFILTRATION BASIN SIZE TO TREAT WQMP DESIGN STORM EVENT AND RETAIN THE 100-YR STORM EVENT
 - ④ PROPOSED OVERLAND RELEASE PATH OF TRAVEL TO PUBLIC RIGHT OF WAY

PRELIMINARY ESTIMATED EARTHWORK QUANTITIES

CUT: 6,199 CY
 FILL: 12,589 CY
 NET: 6,390 CY (FILL)

NOTE: THE ABOVE QUANTITIES ARE APPROXIMATE IN PLACE VOLUMES CALCULATED FROM THE EXISTING GROUND TO THE PROPOSED FINISHED GRADE. EXISTING GROUND IS DEFINED BY THE CONTOURS AND SPOT GRADES ON THE BASE SURVEY. PROPOSED FINISHED GRADE IS DEFINED AS THE FINAL GRADE AS INDICATED ON THE GRADING PLAN(S).

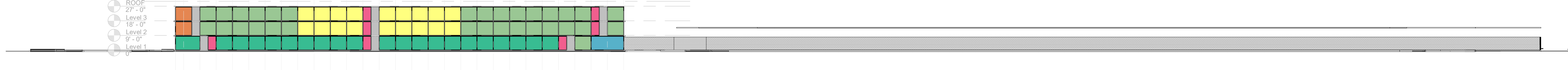
THE EARTHWORK QUANTITIES ABOVE ARE FOR PERMIT PURPOSES ONLY. THEY HAVE NOT BEEN FACTORED TO ACCOUNT FOR CHANGES IN VOLUME DUE TO BULKING, CLEARING AND GRUBBING, SHRINKAGE, OVER-EXCAVATION AND RE-COMPACTION, AND CONSTRUCTION METHODS. NOR DO THEY ACCOUNT FOR THE THICKNESS OF PAVEMENT SECTIONS, FOOTINGS, SLABS, REUSE OF PULVERIZED MATERIALS THAT WILL UNDERLIE NEW PAVEMENTS, ETC. THE CONTRACTOR SHALL RELY ON THEIR OWN EARTHWORK ESTIMATES FOR BIDDING PURPOSES.



PRELIMINARY GRADING AND DRAINAGE PLAN		KHA PROJECT 197425015
SECURE SPACE SELF STORAGE		DATE 3/17/22
INSITE PROPERTY GROUP		SCALE AS SHOWN
CITY OF PALM SPRINGS		DESIGNED BY BH
PREPARED FOR INSITE PROPERTY GROUP		DRAWN BY TC
SHEET NUMBER C200		CHECKED BY BH
		PRELIMINARY FOR REVIEW ONLY NOT FOR CONSTRUCTION PURPOSES Engineer: JENNIFER ZIEBEL P.E. No. 44222 Date: 03/17/22
		© 2022 KIMLEY-HORN AND ASSOCIATES, INC. 4637 CHABOT DRIVE, SUITE 300, PLEASANTON, CA 94588 PHONE: 925-398-4840 FAX: 925-398-4849 WWW.KIMLEY-HORN.COM
REVISIONS No. _____ DATE _____		BY _____ DATE _____

CC BB AA Z Y X W V U T S R Q P O N M L K J I H G F E D C B A

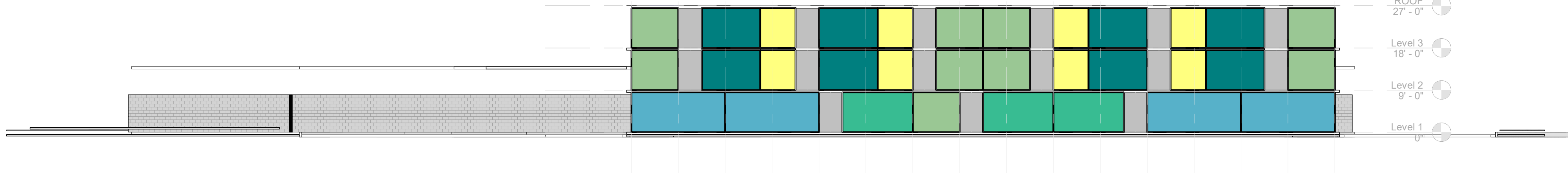
PARAPET
39'-0"
ROOF
27'-0"
Level 3
18'-0"
Level 2
9'-0"
Level 1
0'



1 Section 1
SCALE: 1" = 30'-0"

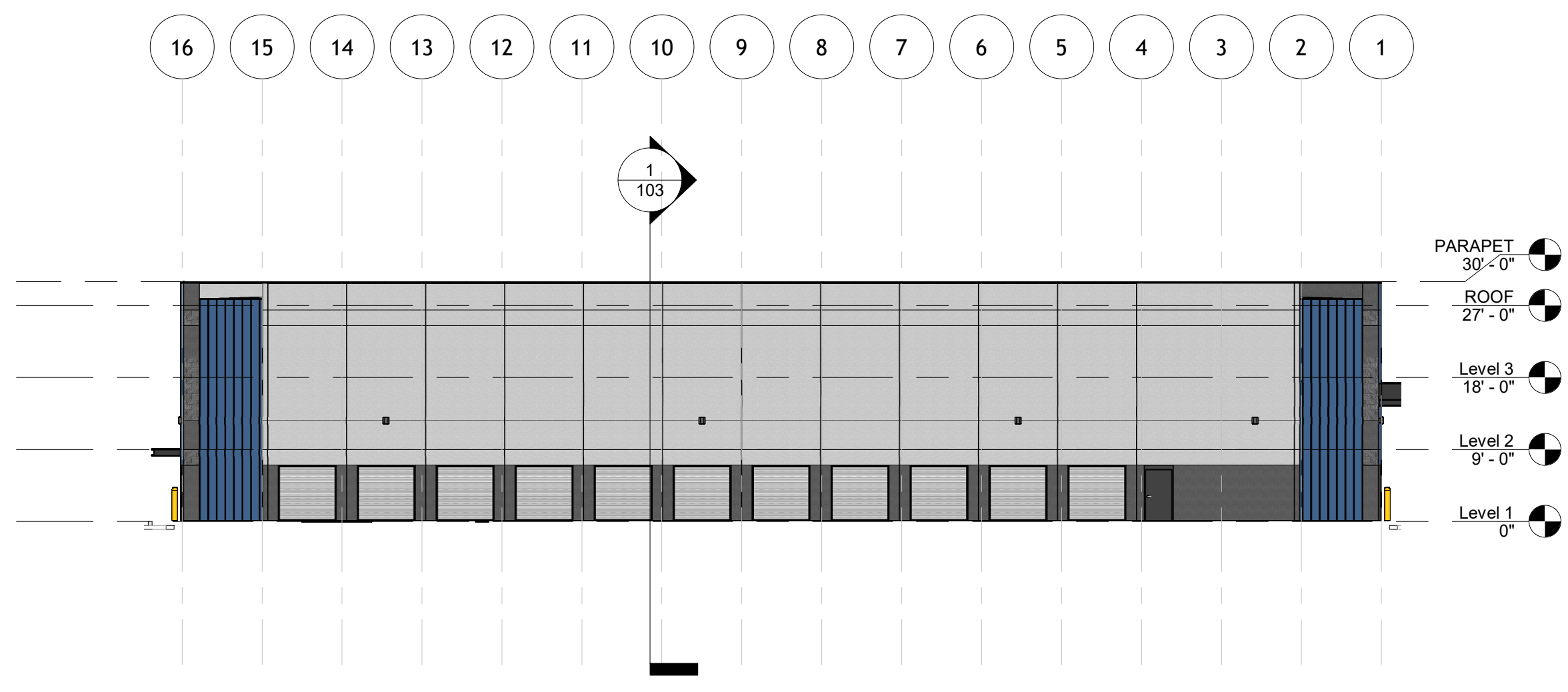
1 2 3 4 5 6 7 8 9 10 11 12 13 14 15 16

PARAPET
39'-0"
ROOF
27'-0"
Level 3
18'-0"
Level 2
9'-0"
Level 1
0'

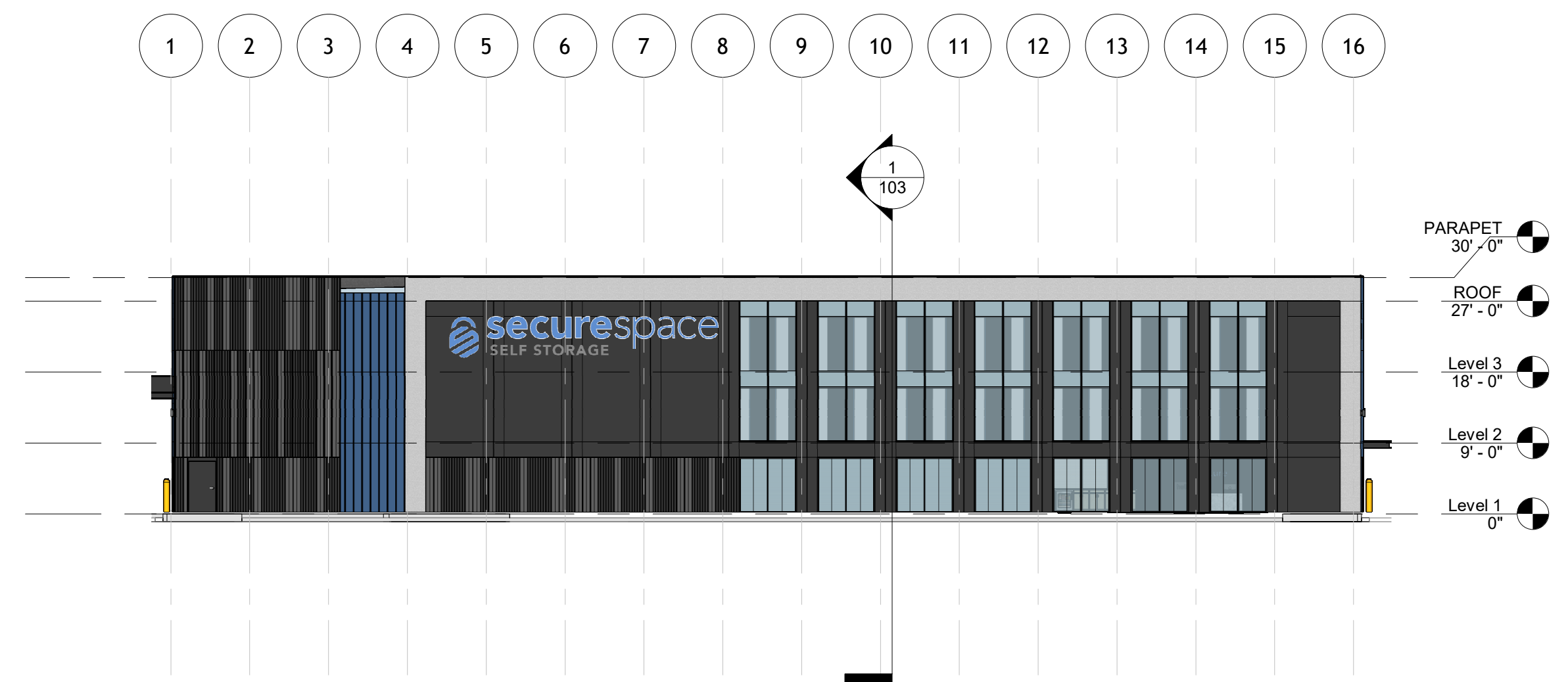


2 Section 2
SCALE: 1/16" = 1'-0"

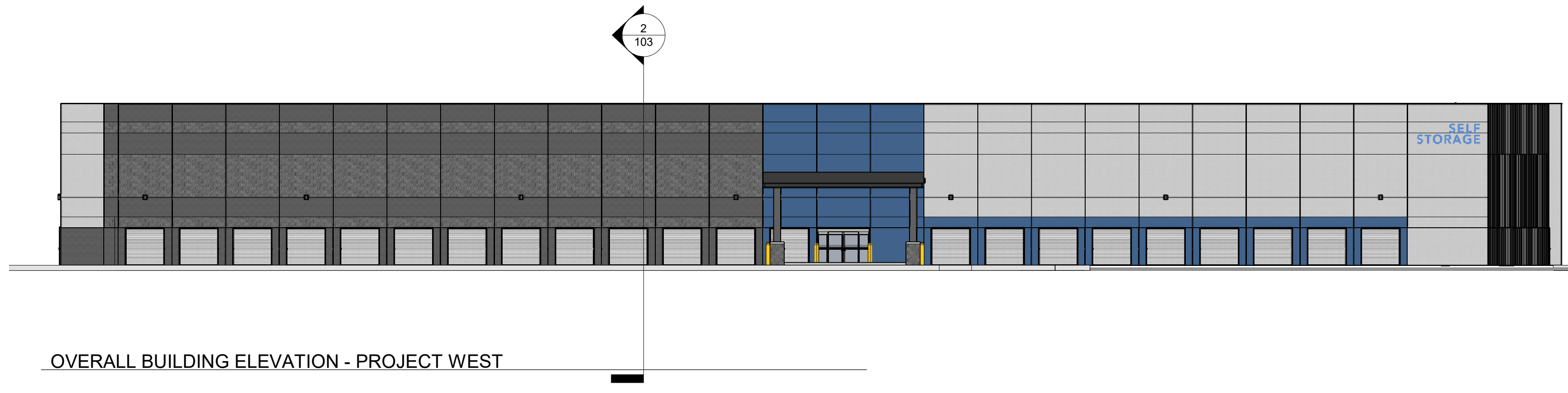
- BLACK: SHERWIN WILLIAMS SW7069 "IRON ORE"
- DARK GREY: SHERWIN WILLIAMS SW7067 "CITYSCAPE"
- MEDIUM GREY: SHERWIN WILLIAMS SW7058 "MAGNETIC GREY"
- WHITE: SHERWIN WILLIAMS SW6238 "ICICLE"
- SECURE SPACE BLUE: SHERWIN WILLIAMS SW6523 "DENIM"
- EXISTING DOORS (BLUE): SHERWIN WILLIAMS SW6524 "COMMODORE BLUE"
- EXISTING DOORS (WHITE): SHERWIN WILLIAMS SW7006 "EXTRA WHITE"
- CMU SMOOTH - LIGHT GREY: ANGELUS BLOCK CO. "SILVER PRECISION"
- CMU BURNISHED - LIGHT GREY: ANGELUS BLOCK CO. "SILVER BURNISHED"
- CMU SHOTBLAST - LIGHT GREY: ANGELUS BLOCK CO. "SILVER SHOTBLAST"
- CMU SPLITFACE - LIGHT GREY: ANGELUS BLOCK CO. "SILVER SHOTBLAST"
- CMU SMOOTH - MEDIUM GREY: ANGELUS BLOCK CO. "GREYSTONE PRECISION"
- CMU BURNISHED - MEDIUM GREY: ANGELUS BLOCK CO. "GREYSTONE BURNISHED"
- CMU SHOTBLAST - MEDIUM GREY: ANGELUS BLOCK CO. "GREYSTONE SHOTBLAST"
- CMU SPLITFACE - MEDIUM GREY: ANGELUS BLOCK CO. "GREYSTONE SPLITFACE"
- INSULATED METAL PANEL - DARK GREY: KINGSPAN "WEATHERED ZINC"
- INSULATED METAL PANEL - LIGHT GREY: KINGSPAN "SILVERSMITH"



OVERALL BUILDING ELEVATION - PROJECT NORTH



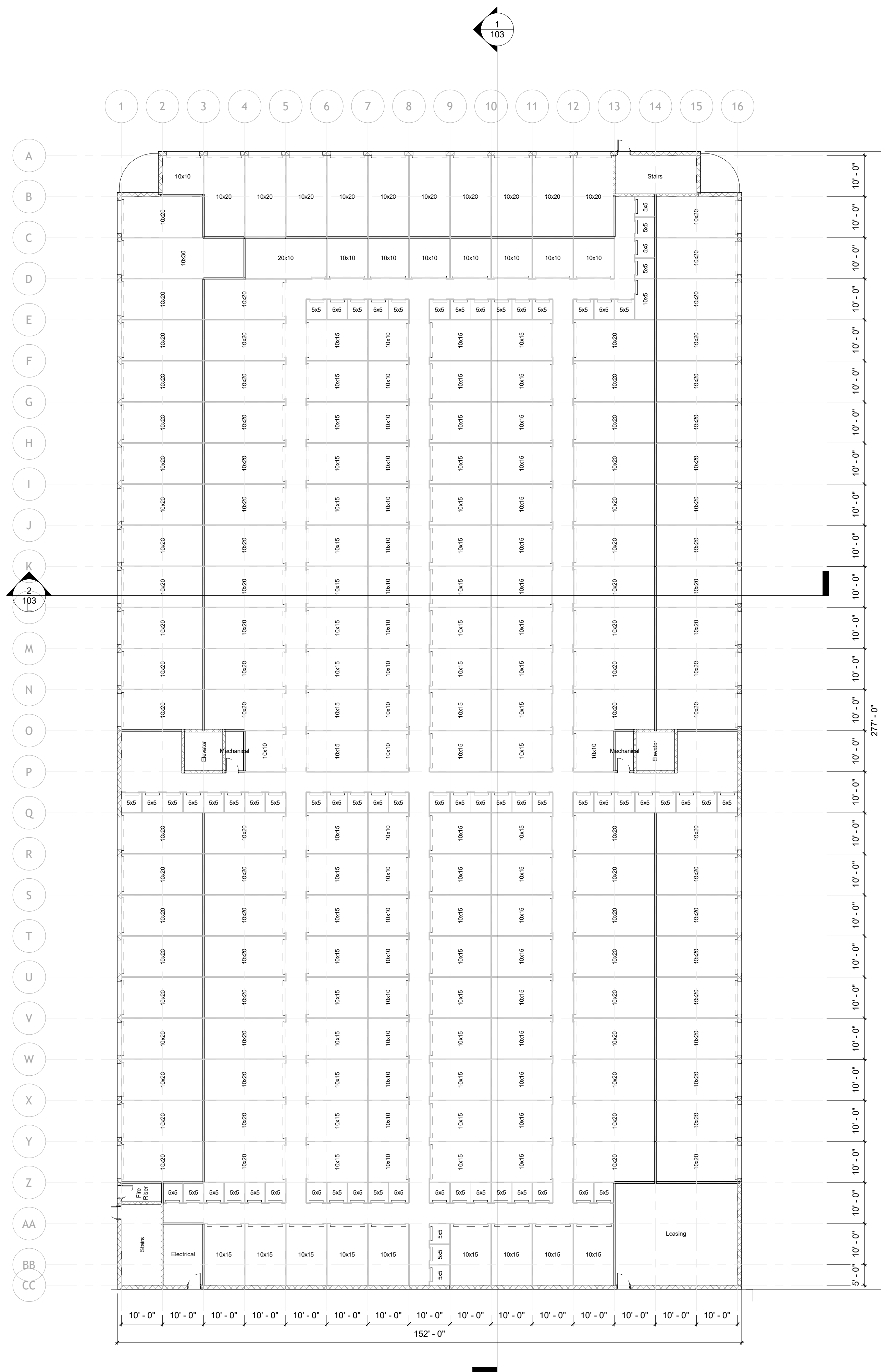
OVERALL BUILDING ELEVATION - PROJECT SOUTH

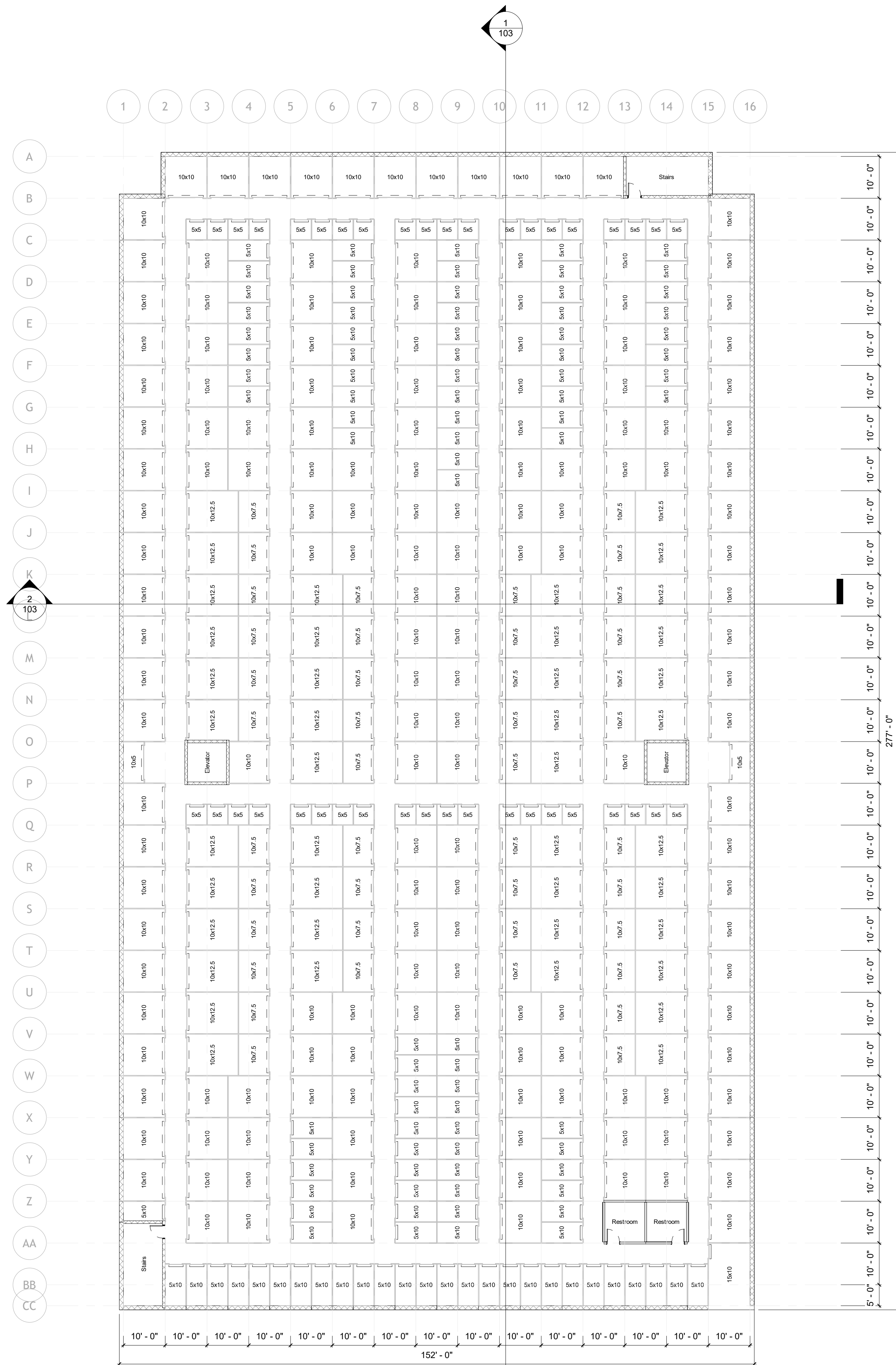


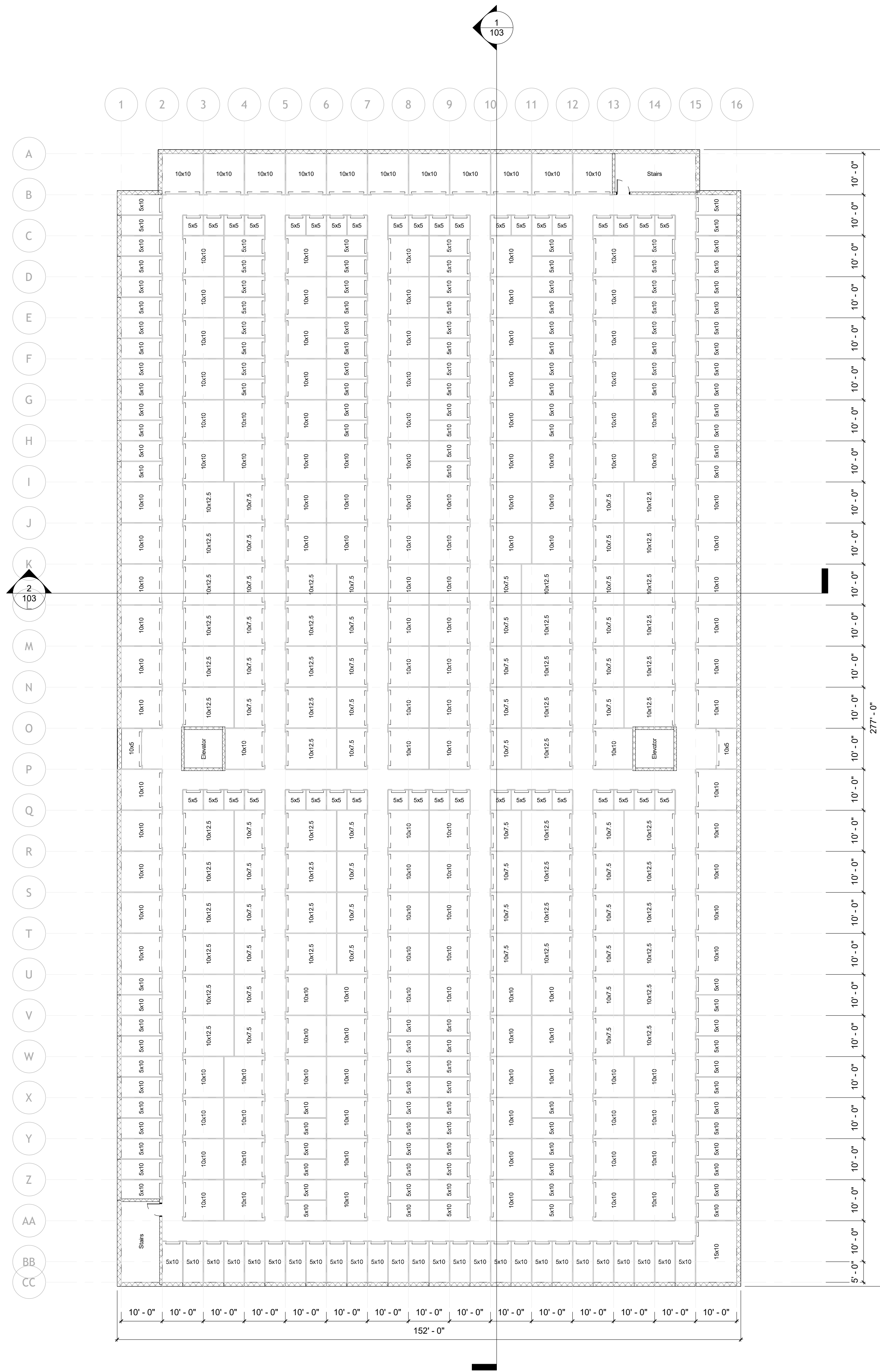
OVERALL BUILDING ELEVATION - PROJECT WEST



OVERALL BUILDING ELEVATION - PROJECT EAST







PAGE BREAK





RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

May 27, 2022

CHAIR
Steve Manos
Lake Elsinore

Deborah Bradford, Project Planner
Riverside County Planning Department
4080 Lemon Street, 12th Floor
Riverside CA 92501

VICE CHAIR
Russell Betts
Desert Hot Springs

COMMISSIONERS

Vacant

File No.: ZAP1069HR22
Related File No.: CUP220010 (Conditional Use Permit)
APN: 458-224-010

John Lyon
Riverside

Steven Stewart
Palm Springs

Dear Ms. Bradford:

Richard Stewart
Moreno Valley

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed County of Riverside Case No. CUP220010 (Conditional Use Permit), a proposal to establish a cannabis distribution and retail facility within an existing 1,553 square foot building on 0.48 acres located at 33671 Highway 74.

Michael Geller
Riverside

STAFF

Director
Paul Rull

The site is located within Airport Compatibility Zone E of the Hemet-Ryan Airport Influence Area (AIA), which does not restrict non-residential intensity.

Simon Houseman
Barbara Santos
Jackie Vega

The elevation of Runway 5-23 at its existing southwesterly terminus is approximately 1,499 feet above mean sea level (AMSL). At a distance of approximately 13,591 feet from the runway, FAA review would be required for any structures with top of roof exceeding 1,635 feet AMSL. The elevation of the project site is 1,624 feet AMSL, and the existing structure is 12 feet, for a maximum top point elevation of 1,636 feet AMSL. There is no proposal to increase the existing building height. Therefore, Federal Aviation Administration (FAA) obstruction evaluation review for height/elevation reasons is not required.

County Administrative Center
4080 Lemon St., 14th Floor.
Riverside, CA 92501
(951) 955-5132

www.rcaluc.org

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2017 Hemet-Ryan Airport Land Use Compatibility Plan, provided that the County of Riverside applies the following recommended conditions:

CONDITIONS:

1. Any outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky.
2. The following uses shall be prohibited:
 - (a) Any use or activity which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.

- (b) Any use or activity which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, outdoor production of cereal grains, sunflower, and row crops, composting operations, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, and construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Hazards to flight.
3. The attached notice of airport in vicinity shall be provided to all potential purchasers of the property.

If you have any questions, please contact Paul Rull, ALUC Director at (951) 955-6893.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Paul Rull, ALUC Director

Attachment: Notice of Airport in Vicinity

cc: Paradise Smoke Shop (applicant)
PGN (representative)
Fivyan Fathy Qasem (property owner)
Angela Jamison, County Airports Manager
ALUC Case File

X:\AIRPORT CASE FILES\Hemet-Ryan\ZAP1069HR22\ZAP1069HR22.LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

THERE IS AN AIRPORT NEARBY.

THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS

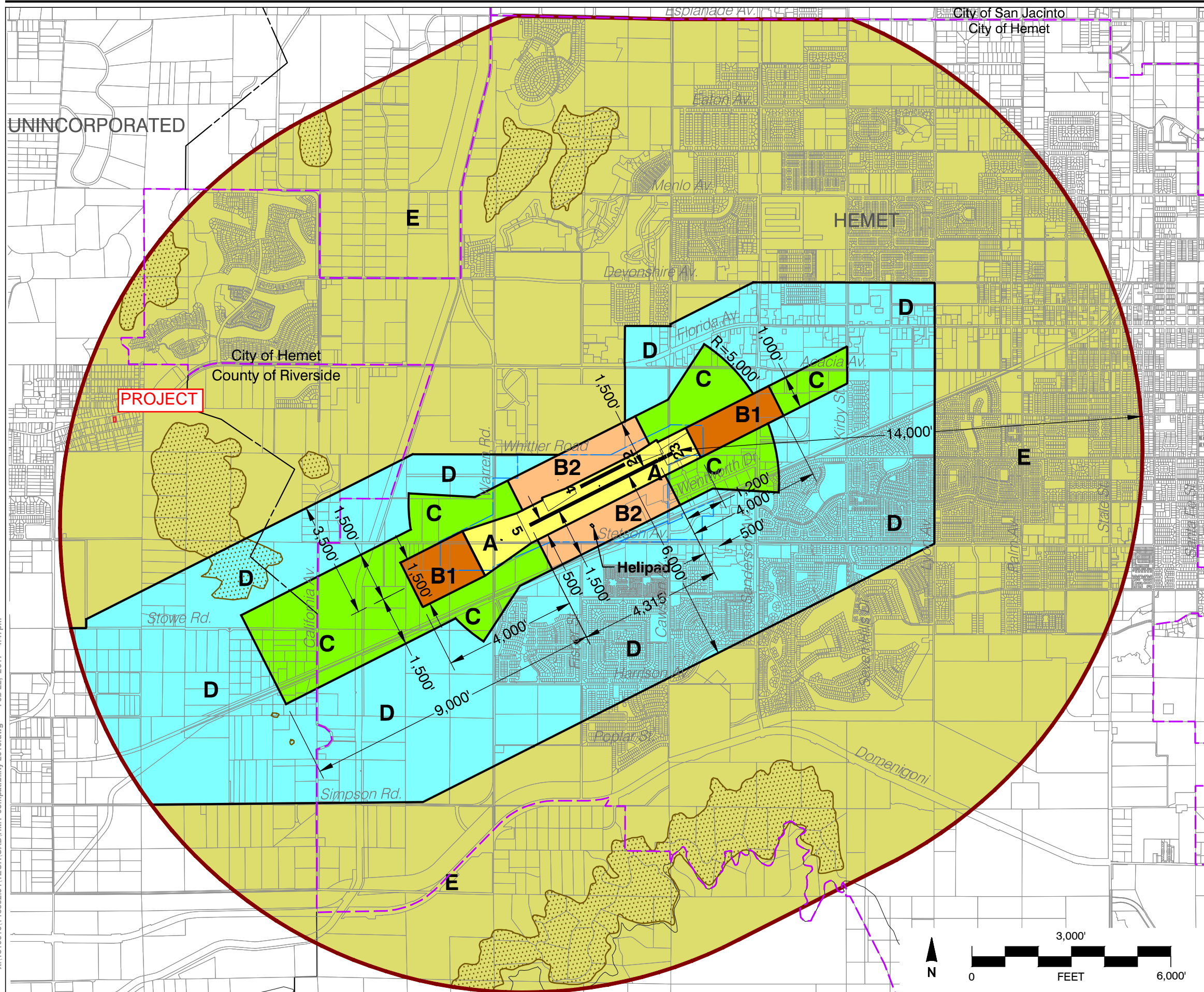
PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



Legend

- Compatibility Zones**
- Airport Influence Area Boundary
 - Zone A
 - Zone B1
 - Zone B2
 - Zone C
 - Zone D
 - Zone E
 - Height Review Overlay Zone

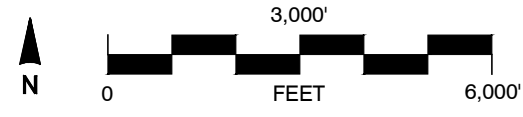
- Boundary Lines**
- Airport Property Line
 - City Limits
 - City Sphere of Influence

Note
 Airport Influence Area boundary measured from a point 200 feet beyond ends of proposed future 4,815 foot runway in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from ends and centerlines of existing 4,315 foot runway.

Riverside County
Airport Land Use Commission
Hemet-Ryan Airport
Land Use Compatibility Plan
 (Adopted February 9, 2017)

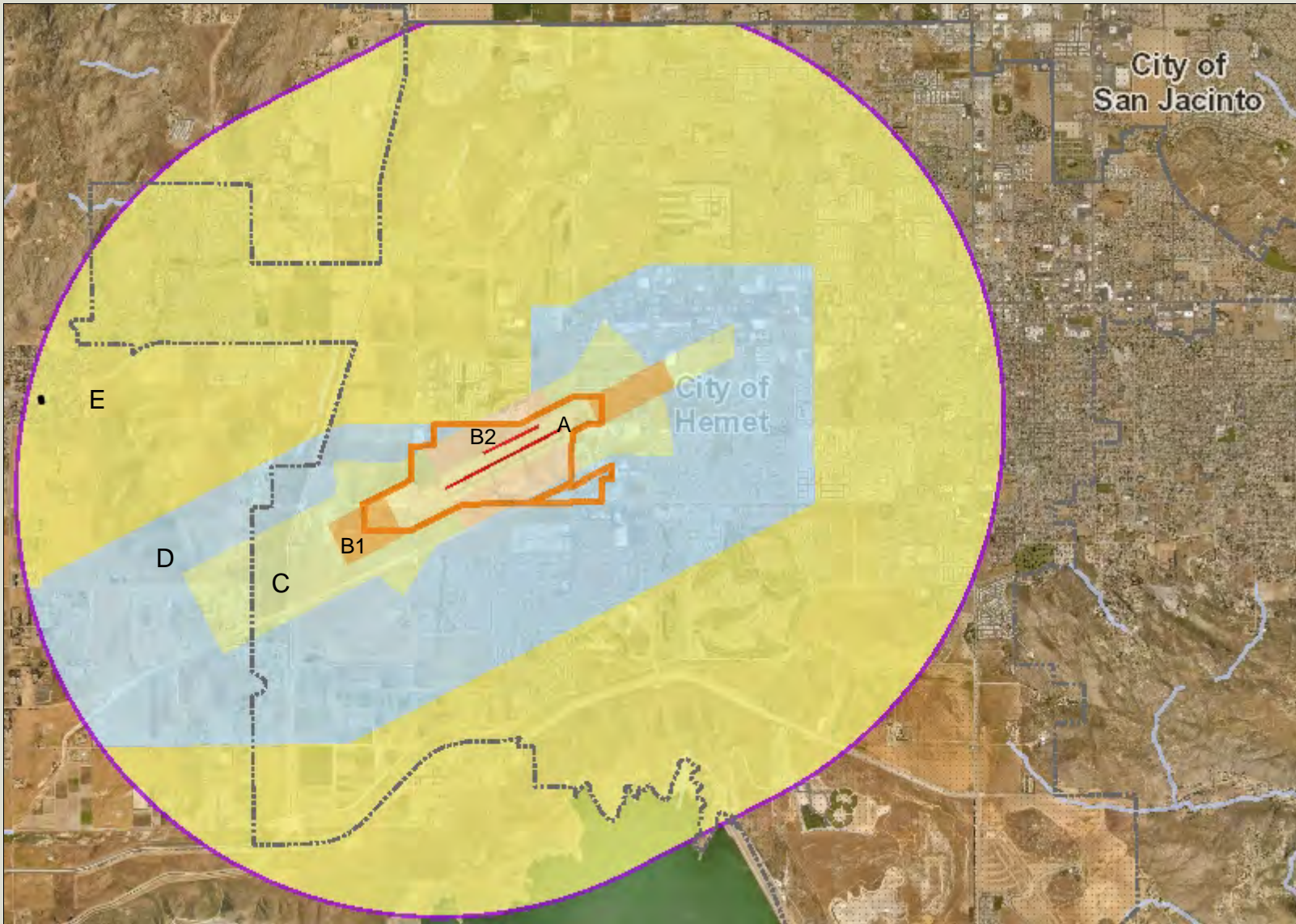
Map HR-1

Compatibility Map
Hemet-Ryan Airport



X:\1819010\113532.01\TECH\CAD\HMT-compatibility 2016.dwg Feb 22, 2017 - 4:47pm

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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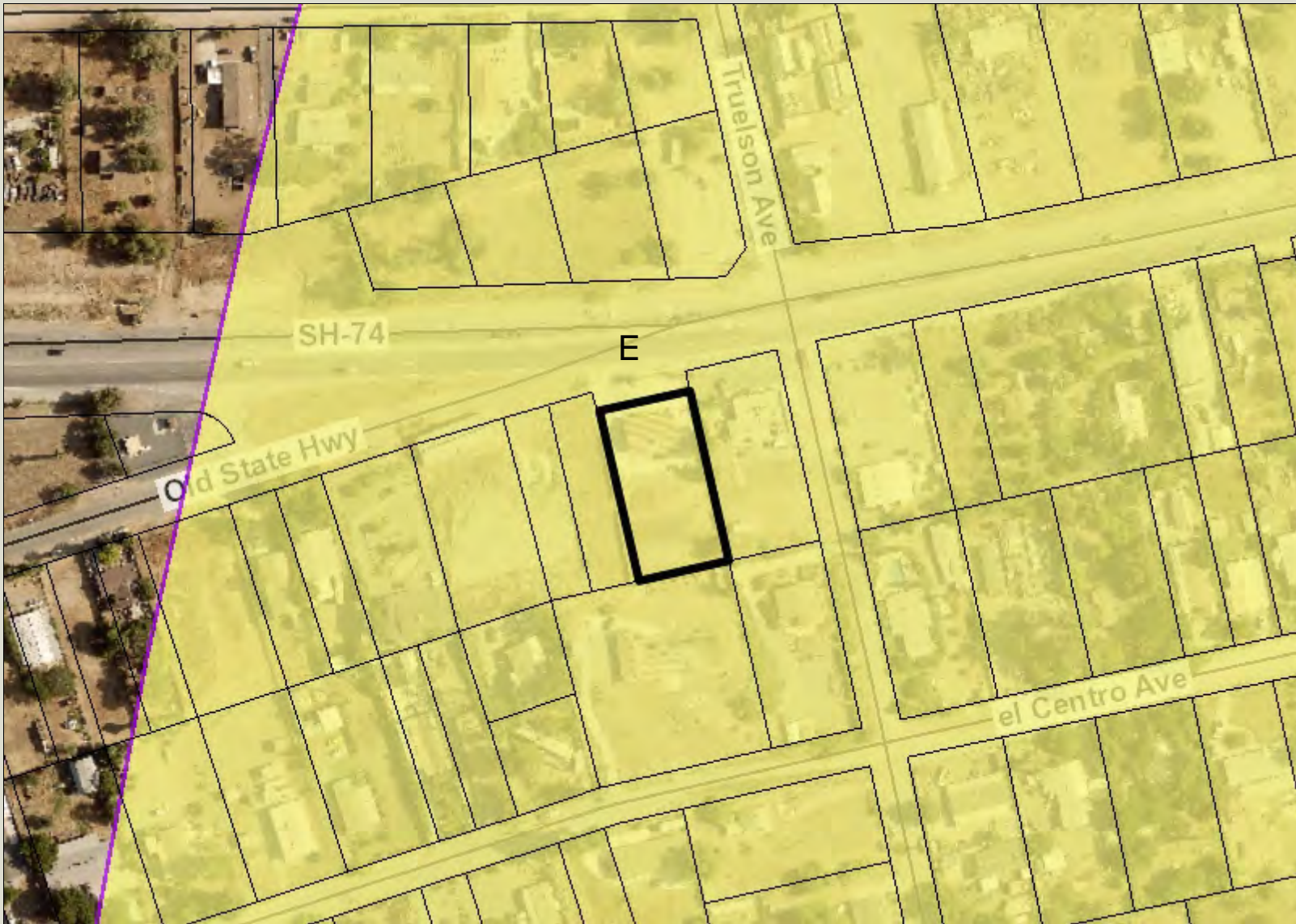


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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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Notes

Map My County Map



- Legend**
- Parcels
 - County Centerline Names
 - County Centerlines
 - Blueline Streams
 - City Areas
 - World Street Map



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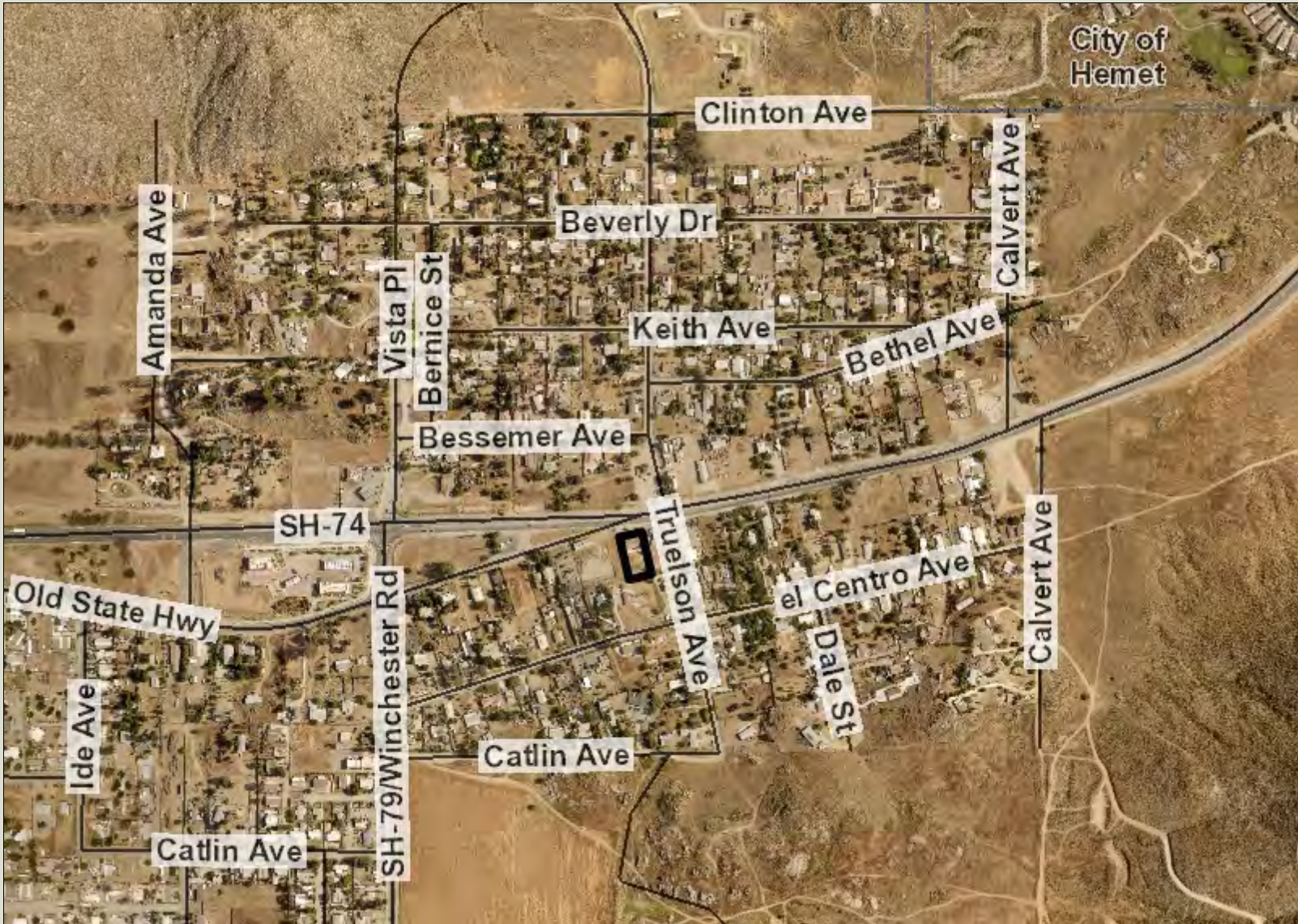


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Notes

Map My County Map



- Legend**
- County Centerline Names
 - County Centerlines
 - Blueline Streams
 - City Areas
 - World Street Map



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Notes



Map My County Map



- Legend**
- Parcel APNs
 - Parcels
 - County Centerline Names
 - County Centerlines
 - Blueline Streams
 - City Areas
 - World Street Map



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Notes

Map My County Map



Legend

-  Blueline Streams
-  City Areas
-  World Street Map



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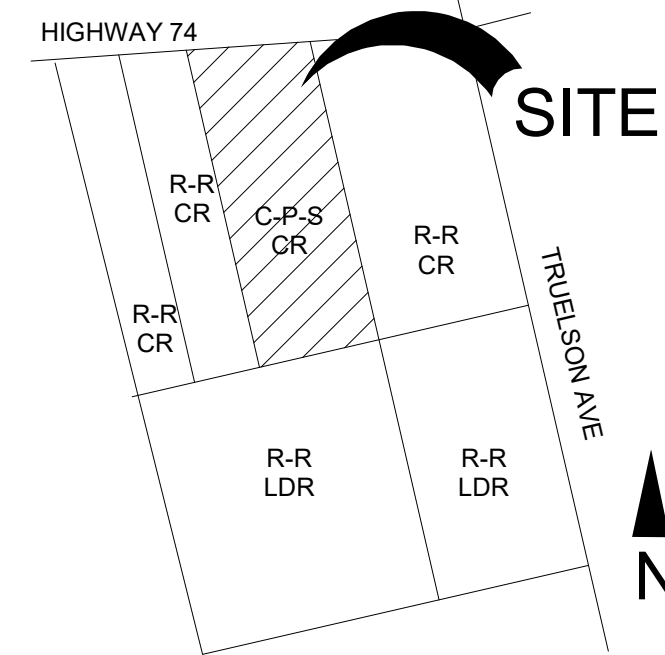
© Riverside County GIS

Notes

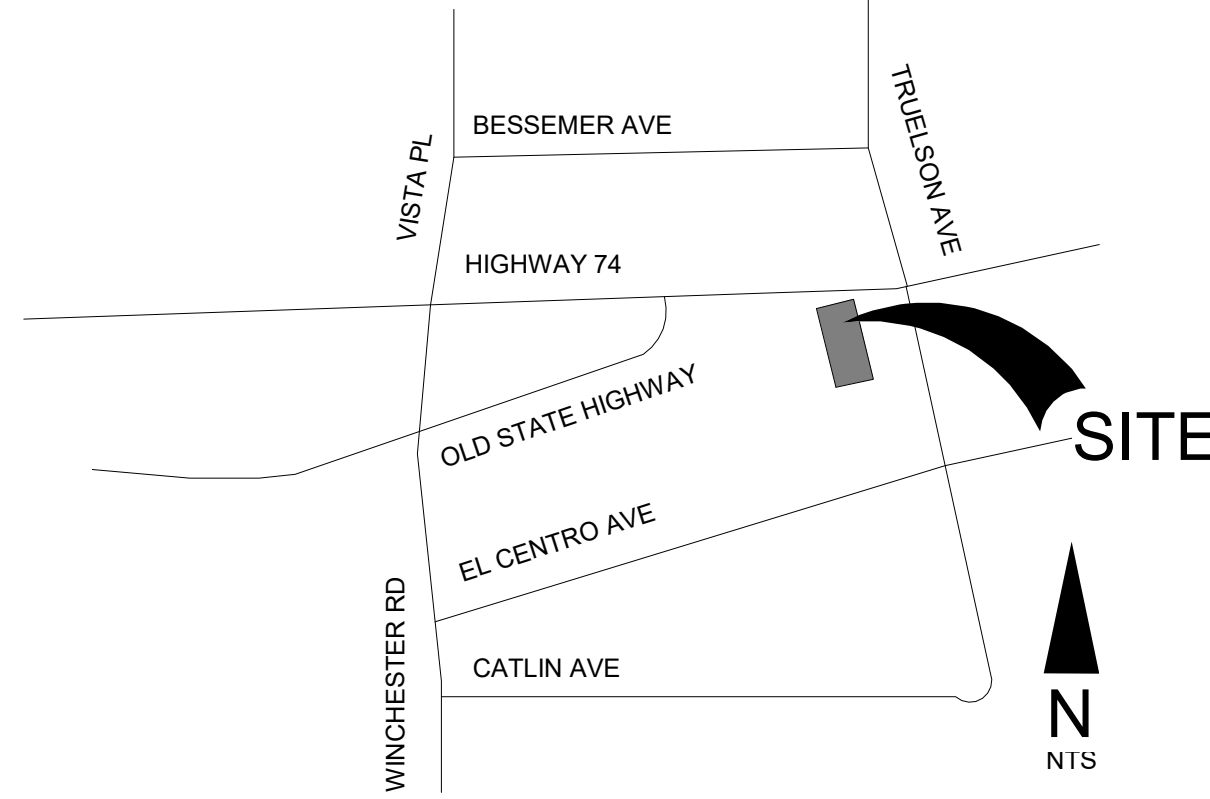
COMMERCIAL CONDITIONAL USE PERMIT
CANNABIS RETAIL FACILITY FOR
33671 HIGHWAY 74
HEMET, CA 92545

EXHIBIT REQUIREMENTS

1. TITLE OF EXHIBIT: CONDITIONAL USE PERMIT
2. PROJECT DIRECTORY
APPLICANT
SHADEN SALAH
2281 W. ESPLANADE AVE.
SUITE 102B
SAN JACINTO, CA
(951) 956-5162
EXHIBIT PREPARER
HERRON + RUMANSOFF ARCHITECTS
530 ST. JOHN PL
HEMET, CA 92543
(951) 652-4431
OWNER
FIVYAN FATHY QASEM
33671 HIGHWAY 74,
HEMET, CA 92545
(951) 923-5182
3. APN: 458-224-010-6
4. SCALE OF EXHIBIT: PER PLAN
5. NORTH ARROW: PER PLAN
6. IS THE PAR FOR A TENTATIVE MAP? NO
7. DIMENSIONS: PER SITE PLAN
GROSS ACREAGE: 0.447 ACRES (19,472 SF)
NET ACREAGE: 0.440 ACRES (19,181 SF)
8. PROJECT BOUNDARY LINES: PER SITE PLAN
9. EXISTING LAND USE: CR
EXISTING ZONING: C-P-S, CZ #2000005
PROPOSED LAND USE: CR
PROPOSED ZONING: C-P-S, CZ #2000005
10. EXISTING AND PROPOSED ZONING AND LAND USE OF SURROUNDING PROPERTIES:



11. EXISTING AND PROPOSED INGRESS & EGRESS: PER SITE PLAN
12. WASTE DISPOSAL SYSTEM PROPOSED: PER SITE PLAN
13. LOCATION AND DIMENSIONS OF EXISTING BUILDINGS: PER SITE PLAN
14. SETBACKS AND PAVED AREAS: PER SITE PLAN
15. OCCUPANCY GROUP AND CONSTRUCTION TYPE: SEE PROJECT DATA
16. VICINITY MAP:



17. CONTOUR LINES: SEE SITE PLAN
18. FEMA MAPPED FLOODPLAINS AND FLOODWAYS INCLUDING ZONE DESIGNATIONS:
OUTSIDE FLOODPLAIN
19. THE ABOVE AND BELOW GROUND LOCATIONS AND AMOUNT OF FLAMMABLE
COMBUSTIBLE LIQUIDS AND WASTE OIL: NONE
20. FOR LAND DIVISIONS:
a. PROPOSED LOT LINES AND APPROXIMATE LOT DIMENSIONS: SEE SITE PLAN
b. PROPOSED BOUNDARY LINES AND APPROXIMATE DIMENSIONS FOR EACH LOT FOR
MOBILE HOME OR RECREATIONAL VEHICLE PARKS: N/A
21. SPECIFIC PLAN: NOT IN A SPECIFIC PLAN
22. FOR CONDOMINIUMS, MOBILE HOME, OR RECREATIONAL VEHICLE PARK: N/A
23. ORDINANCE NO. 469: N/A
24. WATER QUALITY:
25. COUNTY SERVICE AREA: 80 - HOMELAND

26. FLOOD PLAN REVIEW: OUTSIDE FLOODPLAIN
FAULT ZONE: NOT IN A FAULT ZONE
FAULTS: NOT IN A FAULT LINE
LIQUEFACTION POTENTIAL: NOT IN A LIQUEFACTION AREA
27. LOCATION, WIDTHS, AND IMPROVEMENTS OF EXISTING AND PROPOSED PUBLIC UTILITY,
EASEMENTS, TRANSMISSION LINES, POWER AND TELEPHONE POLES, AND UNDERGROUND
UTILITIES ON OR ADJUTING PROPERTY: NONE EXISTING OR PROPOSED.
28. LIST AND ACCURATELY SHOW ALL EASEMENTS OF RECORD: NONE EXISTING OR PROPOSED.

UTILITY PURVEYORS

- TV:** FRONTIER FIOS
(877) 348-9182
32200 JACINTO ST
NUEVO, CA 92567
- TELEPHONE:** FRONTIER
(800) 921-8101
32200 JACINTO ST
NUEVO, CA 92567
- INTERNET:** FRONTIER
(877) 211-1034
32200 JACINTO ST
NUEVO, CA 92567
- ELECTRIC:** SOUTHERN CALIFORNIA EDISON
(800) 655-4555
26100 MENEFEE RD
ROMOLAND, CA 92585
- GAS:** SOUTHERN CALIFORNIA GAS COMPANY
(800) 427-2200
527 N SAN JACINTO ST
HEMET, CA, 92543
- WATER:** EASTERN MUNICIPAL WATER DISTRICT
(800) 426-3693
2270 TRUMBULE RD
PERRIS, CA 92570
- REFUSE:** CR&R
(800) 826-9677
1706 GOETZ RD
PERRIS, CA 92570
- SCHOOL DISTRICT:** HEMET UNIFIED
(951) 765-5100
1791 W ACACIA AVE
HEMET, CA 92545

SHEET INDEX

NO.	SHEET NO.	SHEET NAME
1	T-1.0	TITLESHEET
2	A-0.0	SITE PLAN, FLOOR PLAN & ELEVATIONS
3	1 OF 1	CONCEPTUAL GRADING PLAN

PROJECT/BUILDING DATA

ADDRESS: 33671 HIGHWAY 74 (W FLORIDA AVE)
HEMET, CA 92545
M

OCCUPANCY: M

TYPE OF CONSTRUCTION: V-B (NON-SPRINKLERED)

EXISTING BUILDING S.F.: 1,553 S.F.

LANDSCAPE AREA: 3,109 S.F.

LEGAL DESCRIPTION: LOT 11 IN BLOCK B OF VALLEY VISTA ACRES AS SHOWN BY MAP ON FILE IN BOOK 14, PAGE 1 OF MAPS, RIV. CO. RECORDS. EXCEPTING THEREFROM THE NORTHERLY 20 FEET AS CONVEYED TO THE COUNTY OF RIVERSIDE RECORDED JUNE 19, 1980 AS INST. NO. 112623.

PARKING REQUIREMENTS:

PARKING REQUIRED: 8 (1/200 SF)

PARKING PROVIDED: 19

ACCESSIBLE STALLS REQUIRED: 1 VAN ACCESSIBLE

ACCESSIBLE STALLS PROVIDED: 1 VAN ACCESSIBLE

PROJECT DESCRIPTION

THE PROJECT CONSISTS OF REMODELING EXISTING PARKING LAYOUT AND PROVIDING ACCESSIBLE TRASH ENCLOSURE AND BRING UP TO CODE EXISTING BATHROOM FOR A RETAIL CANNABIS FACILITY. THERE SHALL BE NO CHANGES TO EXISTING FOOTPRINT OF THE BUILDING.

GENERAL SITE NOTES

1. THE PATH OF TRAVEL SLOPE SHALL NOT EXCEED 5% IN THE DIRECTION OF TRAVEL.
2. THE PATH OF TRAVEL CROSS SLOPE SHALL NOT EXCEED 2%.
3. REQUIRED LEVEL LANDINGS AT EXIT DOORS AND RAMP OR STAIR LANDINGS SHALL NOT EXCEED 2% IN ANY DIRECTION. CHANGES IN LEVEL ON GROUND OR FLOOR SURFACES WITHIN AN ACCESSIBLE PATH OF TRAVEL OR MEANS OF EGRESS SHALL COMPLY WITH THE FOLLOWING:
- VERTICAL CHANGES IN LEVEL OF 1/4 INCH (6.4 MM) HIGH MAXIMUM SHALL BE PERMITTED TO BE VERTICAL AND WITHOUT EDGE TREATMENT.
- BEVELED CHANGES IN LEVEL BETWEEN 1/4 INCH (6.4 MM) HIGH MINIMUM AND 1/2 INCH (12.7 MM) HIGH MAXIMUM SHALL BE BEVELED WITH A SLOPE NOT STEEPER THAN 1:2.
- CHANGES IN LEVEL GREATER THAN 1/2 INCH (12.7 MM) HIGH SHALL BE RAMPED.
4. SITE FULLY COMPLIES WITH TITLE 24, ACCESSIBILITY STANDARDS; SUBJECT TO FIELD VERIFICATION
5. THIS PROJECT IS SUBJECT TO TITLE 24, PART 2, OF THE CALIFORNIA CODE REGULATIONS, SECTION 1.9.1 AND 11B-107, THE STATE'S DISABLED ACCESS AND ADAPTABILITY REQUIREMENTS.
- I CERTIFY THAT THE PRIMARY PATH OF TRAVEL TO THE SPECIFIC AREA OF ALTERATION, STRUCTURAL REPAIR OR ADDITION FROM THE PUBLIC WAY OR ACCESSIBLE PARKING SPACE AS INDICATED ON THE PLANS DOES NOT INCLUDE STEPS OR SLOPE EXCEEDING 1:20 EXCEPT WHERE ACCESS IS PROVIDED BY A RAMP WITH 1:12 MAXIMUM SLOPE, ACCESSIBLE ELEVATOR OR OTHERWISE GRANTED BY AN UNREASONABLE HARDSHIP EXEMPTION. I UNDERSTAND THAT IF THE PRIMARY PATH OF TRAVEL IS FOUND NOT TO BE AS INDICATED, SIGNIFICANT DELAYS MAY RESULT.
- FURTHERMORE, I CERTIFY THAT THESE PLANS WERE PREPARED UNDER MY DIRECT SUPERVISION AND THAT THE AREA OF SPECIFIC ALTERATION, STRUCTURAL REPAIR OR ADDITION, INCLUDING A PRIMARY ENTRANCE TO THE EXISTING BUILDING AND, WHEN APPLICABLE, SANITATION FACILITIES, DRINKING FOUNTAINS, SIGNS AND PUBLIC TELEPHONES SERVING THE AREA COMPLIES WITH CURRENT TITLE 24 ACCESSIBILITY REQUIREMENTS.

DESIGNER'S SIGNATURE
RUSSELL RUMANSOFF

ARCHITECT
TITLE
DATE: 08-13-21



All ideas, designs, and arrangements indicated or represented by these drawings are the property of Herron+Rumansoff Architects, Inc. and constitute the original and unpublished work of the Architect and are intended to be used in connection with this specific project only. No copying, reproduction or reuse of these drawings is permissible without the written consent of Herron+Rumansoff Architects, Inc.

Herron + Rumansoff architects, inc.

530 St. Johns Place Hemet, California 92543

project

33671 HIGHWAY 74
HEMET, CA 92545

COMMERCIAL CONDITIONAL USE PERMIT
CANNABIS RETAIL FACILITY FOR

CLIENT REVIEW 12.16.21
issue purpose
date

architect's signature

consultant

revisions

951 652-4431

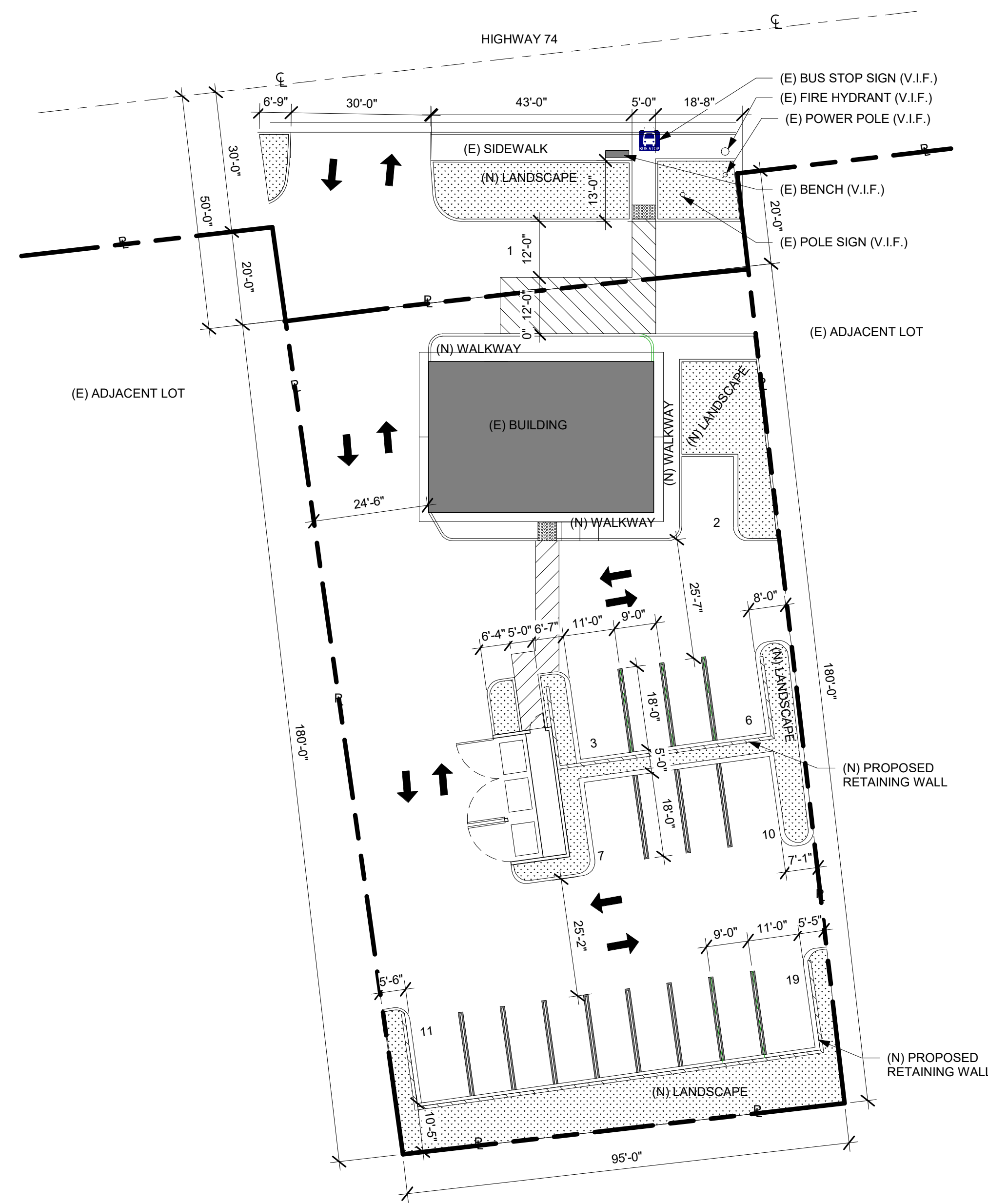
TITLESHEET

1045-15-21

job no. sht. description

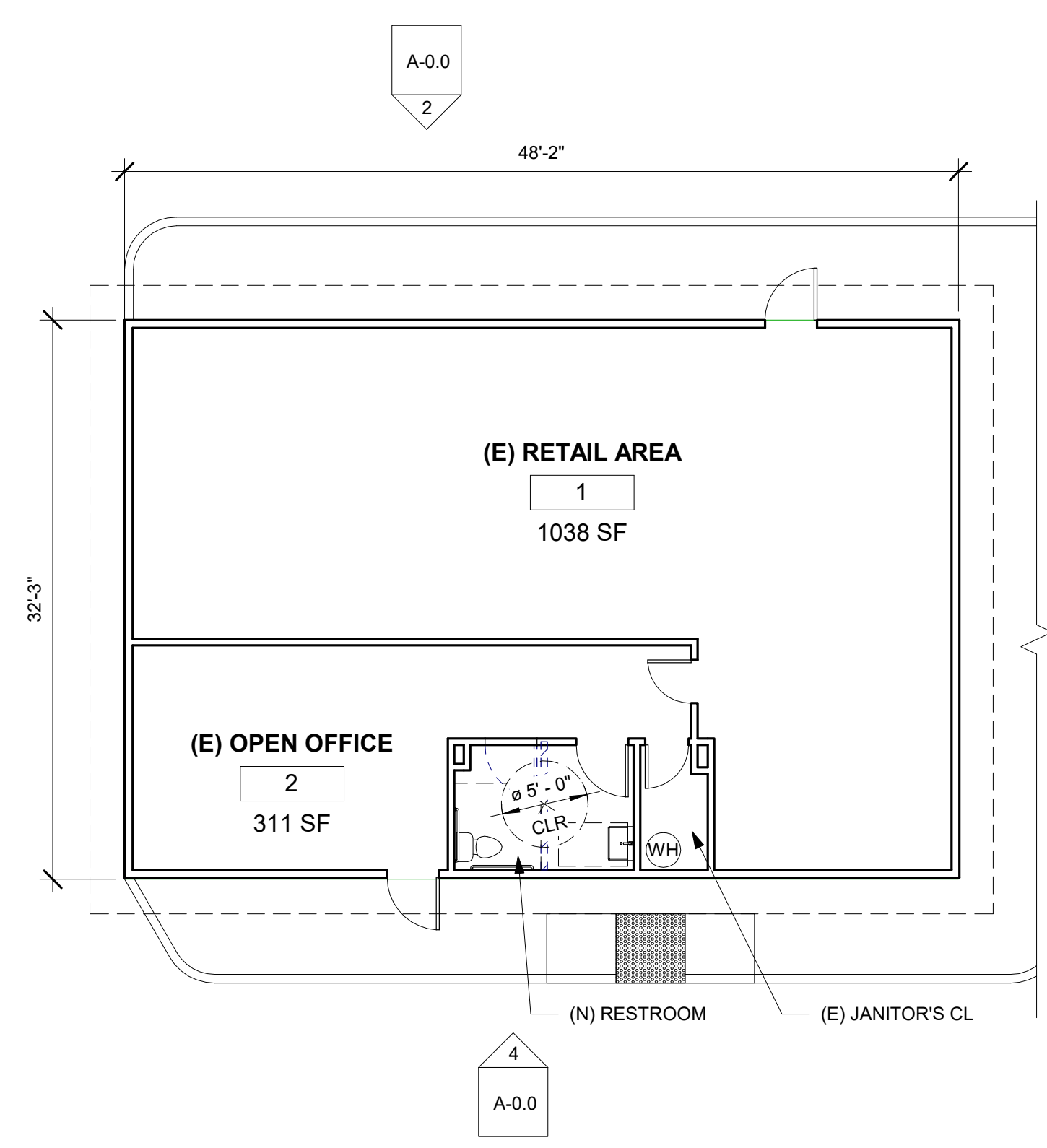
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sheet no.

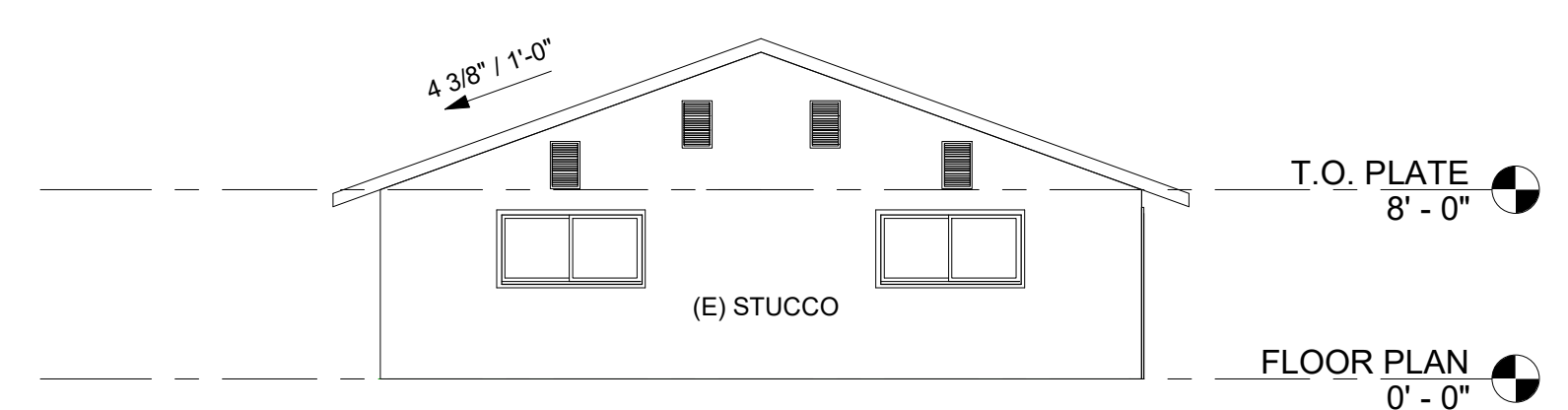


A SITE PLAN
1" = 20'-0"

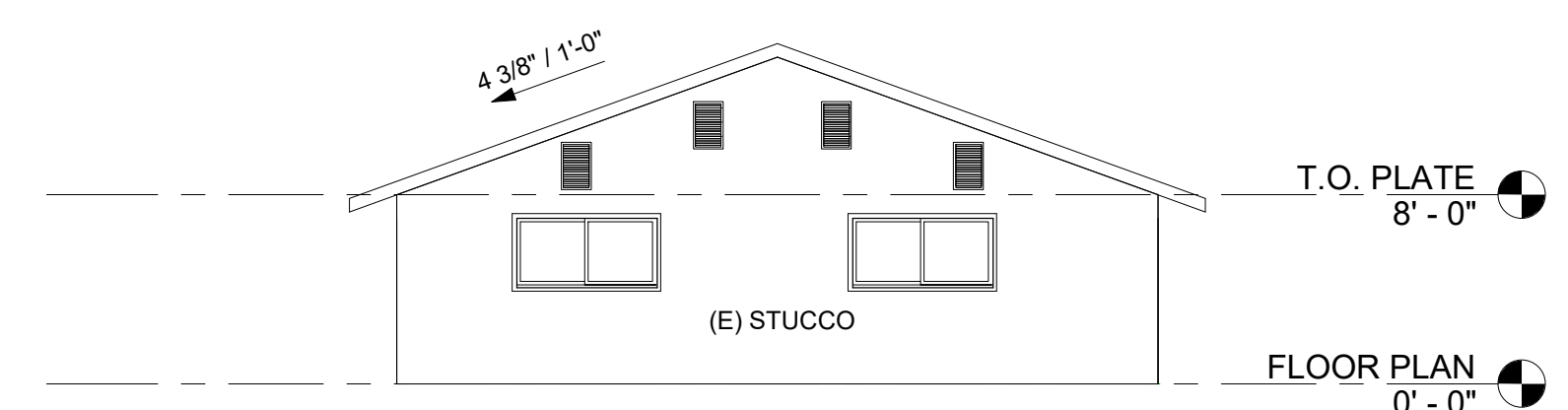
B FLOOR PLAN
1/8" = 1'-0"



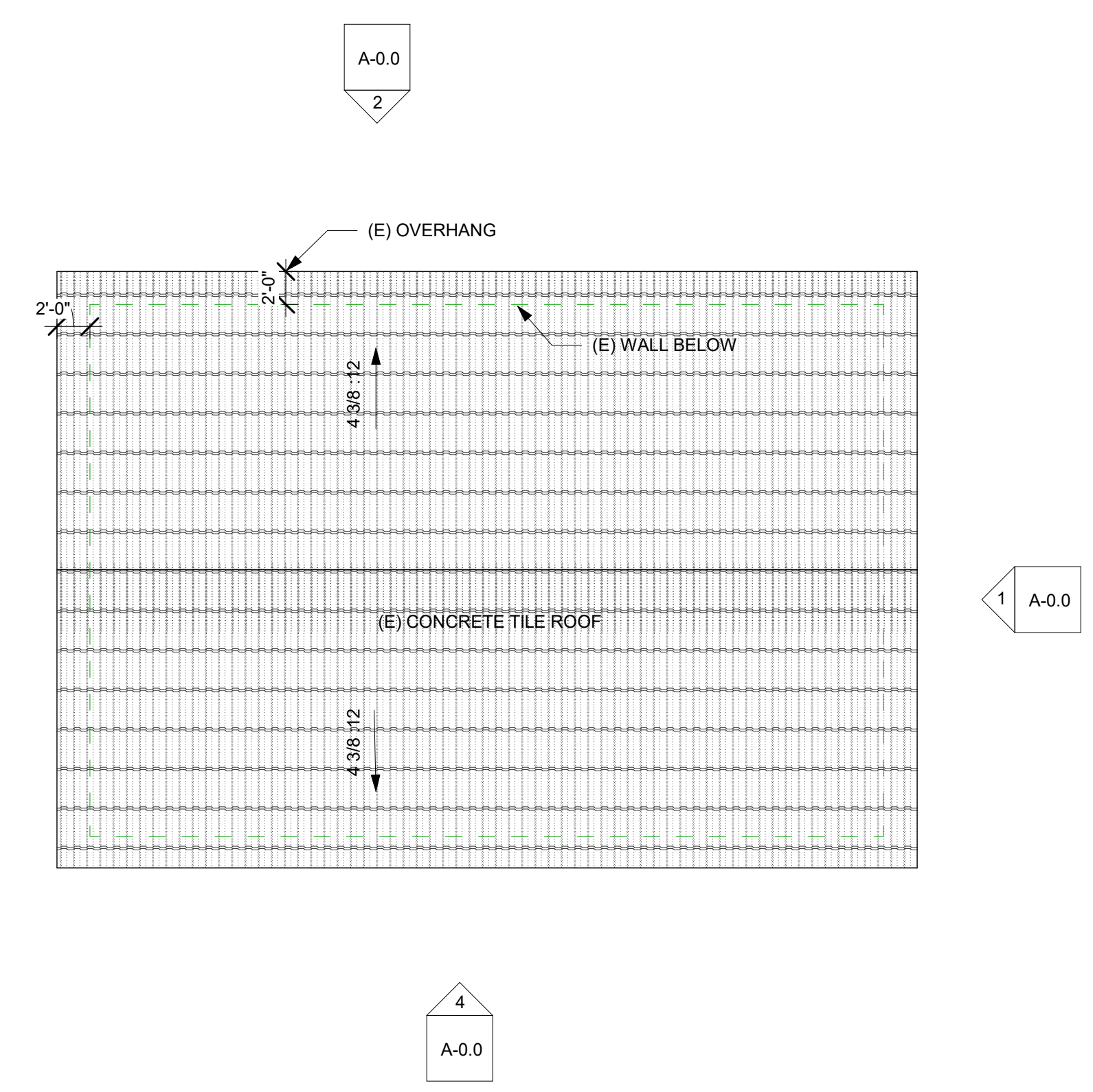
1 EAST ELEVATION
1/8" = 1'-0"



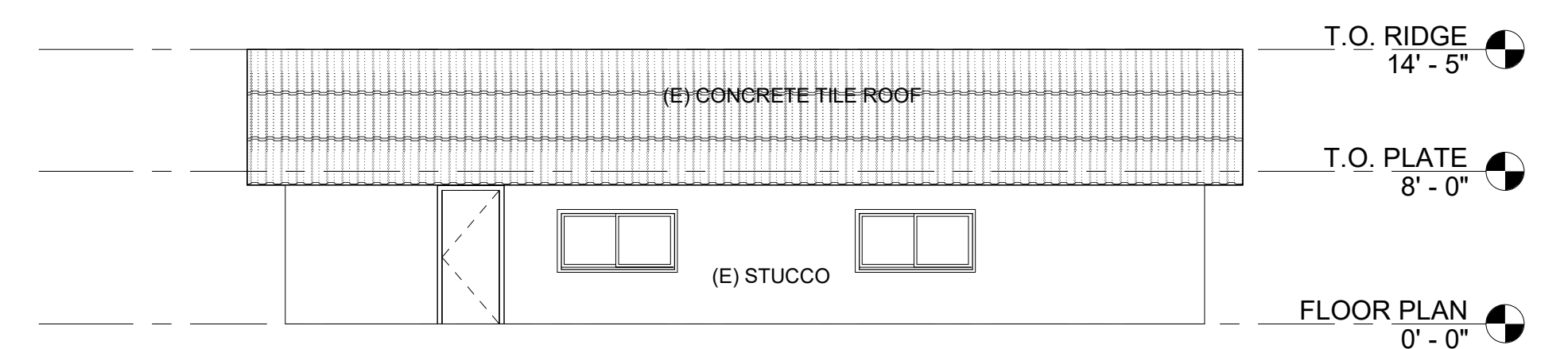
3 WEST ELEVATION
1/8" = 1'-0"



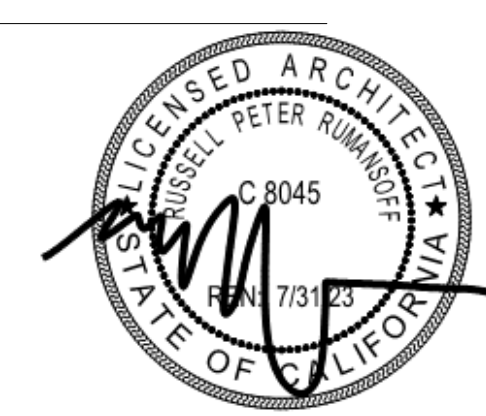
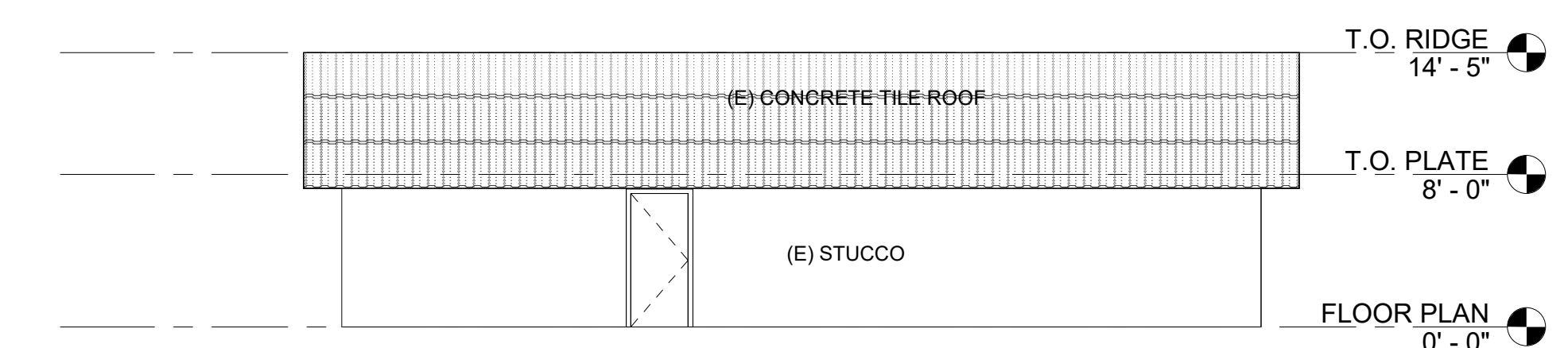
C ROOF PLAN
1/8" = 1'-0"



2 NORTH ELEVATION
1/8" = 1'-0"

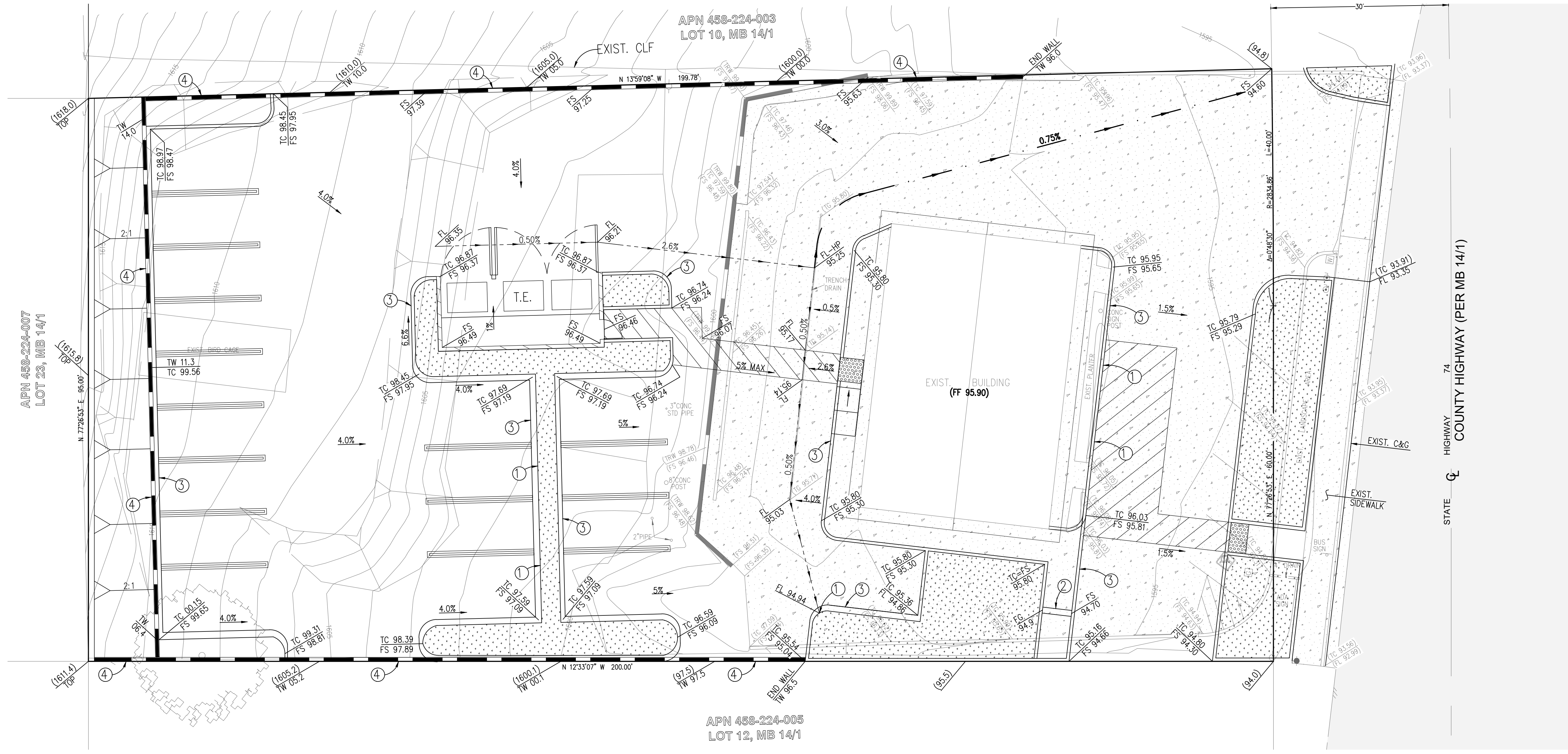
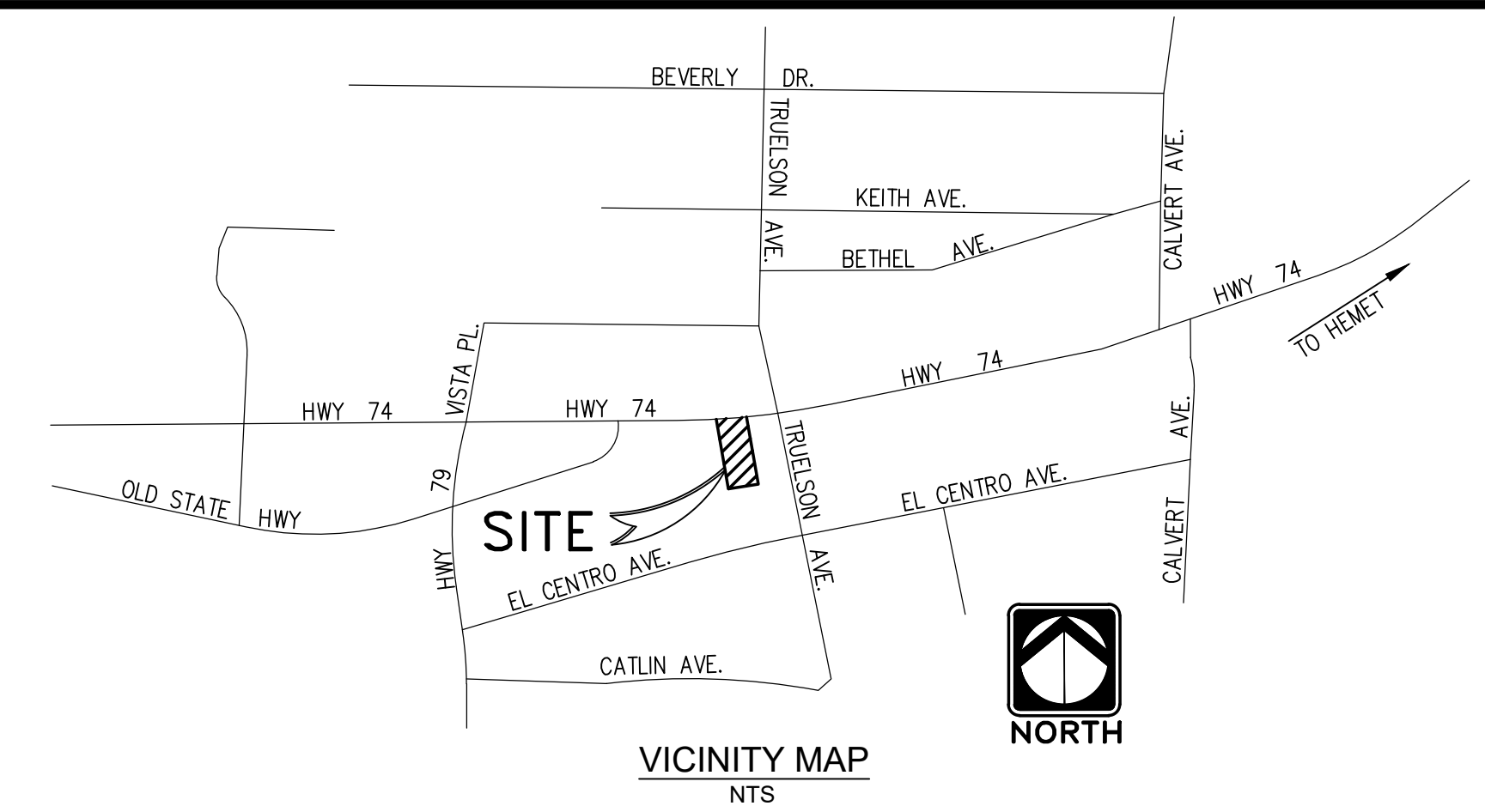
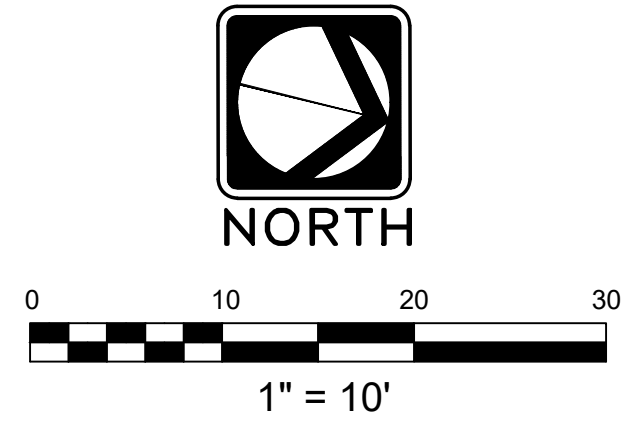


4 SOUTH ELEVATION
1/8" = 1'-0"



heron + rumanosoff
 architects, inc.
 530 St. Johns Place Hemet, California 92343
 project: 1045-15-21
 job no. sht.description
 SITE PLAN, FLOOR PLAN & ELEVATIONS
 A-0.0
 CLIENT REVIEW 12.16.21
 issue purpose date
 architect's signature
 consultant
 revisions
 951 652-4431

All ideas, designs, and arrangements indicated or represented by these drawings are the property of Heron+Rumanosoff Architects, Inc. and constitute the original and unpublished work of the Architect and are intended to be used in connection with this specific project only. No copying, reproduction or reuse of these drawings is permissible without the written consent of Heron+Rumanosoff Architects, Inc.



PROPERTY DESCRIPTION
 LOT 11 IN BLOCK B OF VALLEY VISTA ACRES
 AS SHOWN BY MAP ON FILE IN BOOK 14, PAGE 1
 OF MAPS, RIV. CO. RECORDS.

EXCEPTING THEREFROM THE NORTHERLY 20 FEET
 AS CONVEYED TO THE COUNTY OF RIVERSIDE
 RECORDED JUNE 19, 1980 AS INST. NO. 112623.

ASSESSOR'S PARCEL NO.
 458-224-010

- KEY NOTES**
- CURB OPENINGS FOR DRAINAGE
 - UNDER-SIDEWALK DRAIN
 - CONCRETE CURB
 - RETAINING WALL

SOURCE OF TOPOGRAPHY
 A FIELD SURVEY PERFORMED BY
 BLAINE A. WOMER CIVIL ENGINEERING.
 DATED JUNE 25, 2021.

- LEGEND**
- EXISTING SIGN (AS NOTED)
 - EXISTING SERVICE POLE
 - EXISTING WATER METER
 - EXISTING RETAINING WALL
 - EXISTING FIRE HYDRANT
 - EXISTING 18" DIA. TREE
 - EXISTING CONCRETE SURFACE
 - EXISTING ASPHALT SURFACE
 - EXISTING EDGE OF PAVEMENT
 - EXISTING CHAIN LINK FENCE
 - EXISTING GAS METER
 - EXISTING AV/AR
 - TOP OF CURB
 - TC TOP OF CURB
 - FL FLOW LINE
 - FS FINISHED SURFACE
 - TRW TOP OF EXISTING RETAINING WALL
 - () EXISTING ELEV. INFORMATION
 - PROPOSED LANDSCAPE
 - TOP OF SLOPE
 - TOE OF SLOPE
 - TW TOP OF PROPOSED RETAINING WALL

PRELIMINARY EARTHWORK
 RAW CUT: 2750 C.Y.
 RAW FILL: 0 C.Y.

NOTE:
 EXISTING SITE IMPROVEMENTS ARE
 SHOWN FADED BACK FOR CLARITY

BENCH MARK
 RIVERSIDE COUNTY BENCH MARK M-142, A 3.5" ALUM.
 DISK STAMPED "RIVERSIDE COUNTY BENCH MARK M-142
 1984", FLUSH ON TOP OF THE WLY END OF A 9" WIDE x 10'
 LONG CONCRETE DRAINAGE HEADWALL, AT THE WLY
 SIDE OF THE ENTRANCE TO THE PANORAMA COVE
 MOBILE HOME PARK (VINCENT AVE.), 0.9 OF A MILE WLY
 ALONG STATE HWY. 74 FROM THE INTERSECTION OF
 WINCHESTER ROAD (S.H. 79) AND S.H. 74.
 ELEV. = 1616.305

REC'D	COUNTY OVERSITE ENGINEER	REGISTRATION #	DATE SIGNED
APPROVED AS TO CONFORMANCE WITH APPLICABLE COUNTY STANDARDS AND PRACTICES.			

NOTE:
 WORK CONTAINED WITHIN THESE PLANS
 SHALL NOT COMMENCE UNTIL AN
 ENCROACHMENT PERMIT AND/OR A
 GRADING PERMIT HAS BEEN ISSUED.

The private engineer signing these plans is responsible for assuring the accuracy and acceptability of the design hereon. In the event of discrepancies arising after county approval or during construction, the private engineer shall be responsible for determining an acceptable solution and revising the plans for approval by the county.

MARK	BY	DATE	REVISIONS	APPR.	DATE

SEAL-ENGINEER

BLAINE A. WOMER
 CIVIL ENGINEERING

- PLANNING
- SURVEYING
- CIVIL ENGINEERING
- PUBLIC WORKS

PREPARED BY: _____ R.C.E. NO. **46354**
 DATE _____

BENCHMARK:
 SEE ABOVE

SCALE: _____

COUNTY OF RIVERSIDE
CONCEPTUAL GRADING PLAN
 FOR
CANNABIS RETAIL FACILITY

FDR: _____ W.D. _____ COUNTY FILE NO. _____

SHEET NO.
1
 OF 1 SHTS

COMPREHENSIVE LANDSCAPE PLAN

SOIL TESTING (SOIL MANAGEMENT REPORT): CONTRACTOR SHALL PROVIDE A HORTICULTURAL SOILS ANALYSIS PERFORMED BY A LABORATORY OF THE CALIFORNIA ASSOC. OF AGRICULTURAL LABORATORIES. CONTRACTOR SHALL ADJUST SOIL AMENDMENT RECOMMENDATIONS TO CONFORM TO SOIL ANALYSIS RESULTS AS REQUIRED. SEND THE REPORT TO THE COUNTY ELECTRONICALLY BEFORE THE PRE-LANDSCAPE INSTALLATION INSPECTION.

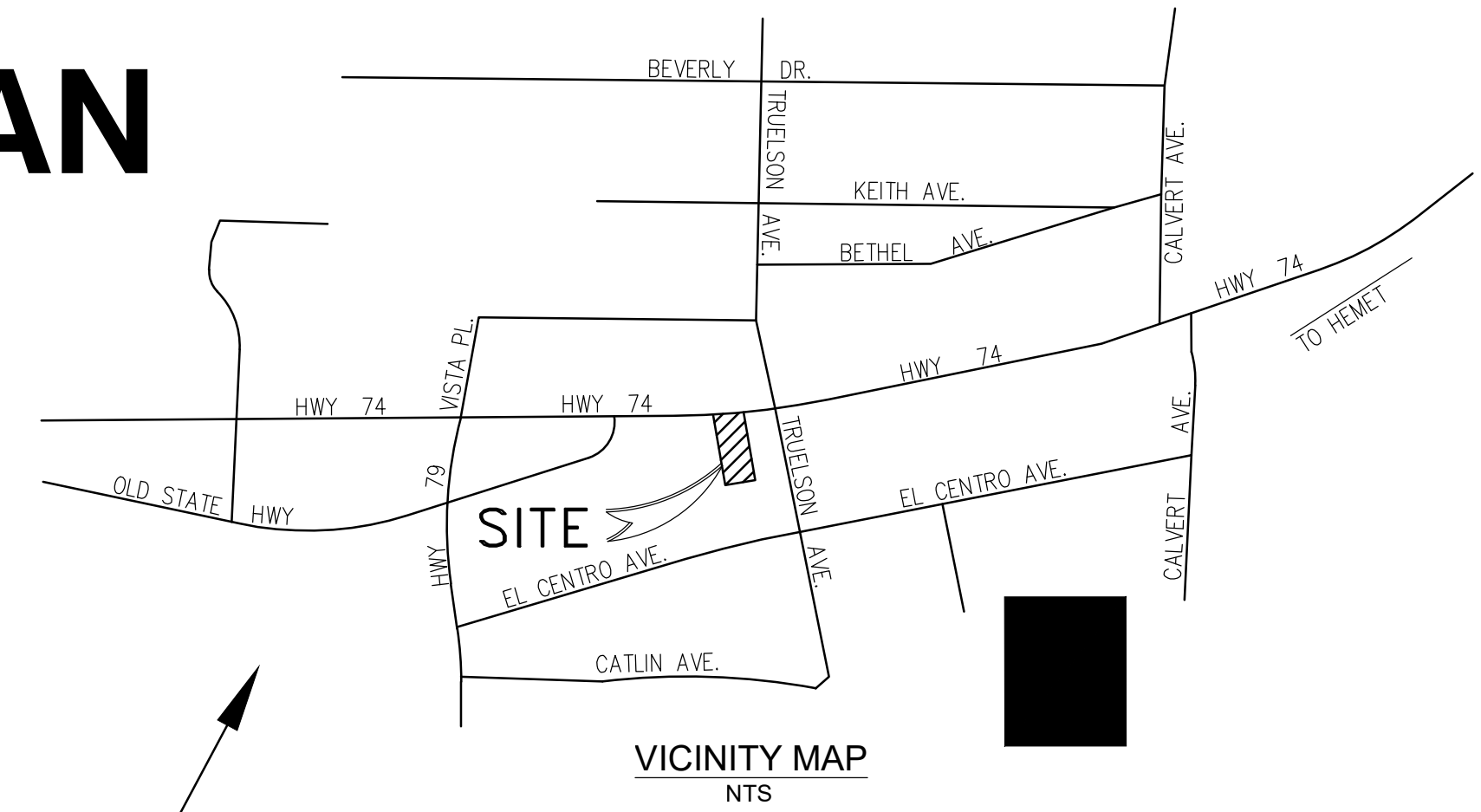
ENCROACHMENT NOTE:
THE CONTRACTOR SHALL OBTAIN ALL ENCROACHMENT & GRADING PERMITS PRIOR TO STARTING ANY WORK.

SHADE REQUIREMENT

REQUIRED 18 SPACES @ 162 S.F. = 2,916 S.F.
2,916 S.F. @ 30% COVERAGE REQUIRED= 875 S.F.
ACTUAL AREA SHADED= 1,495 S.F.

INTERIOR LANDSCAPE REQUIREMENT

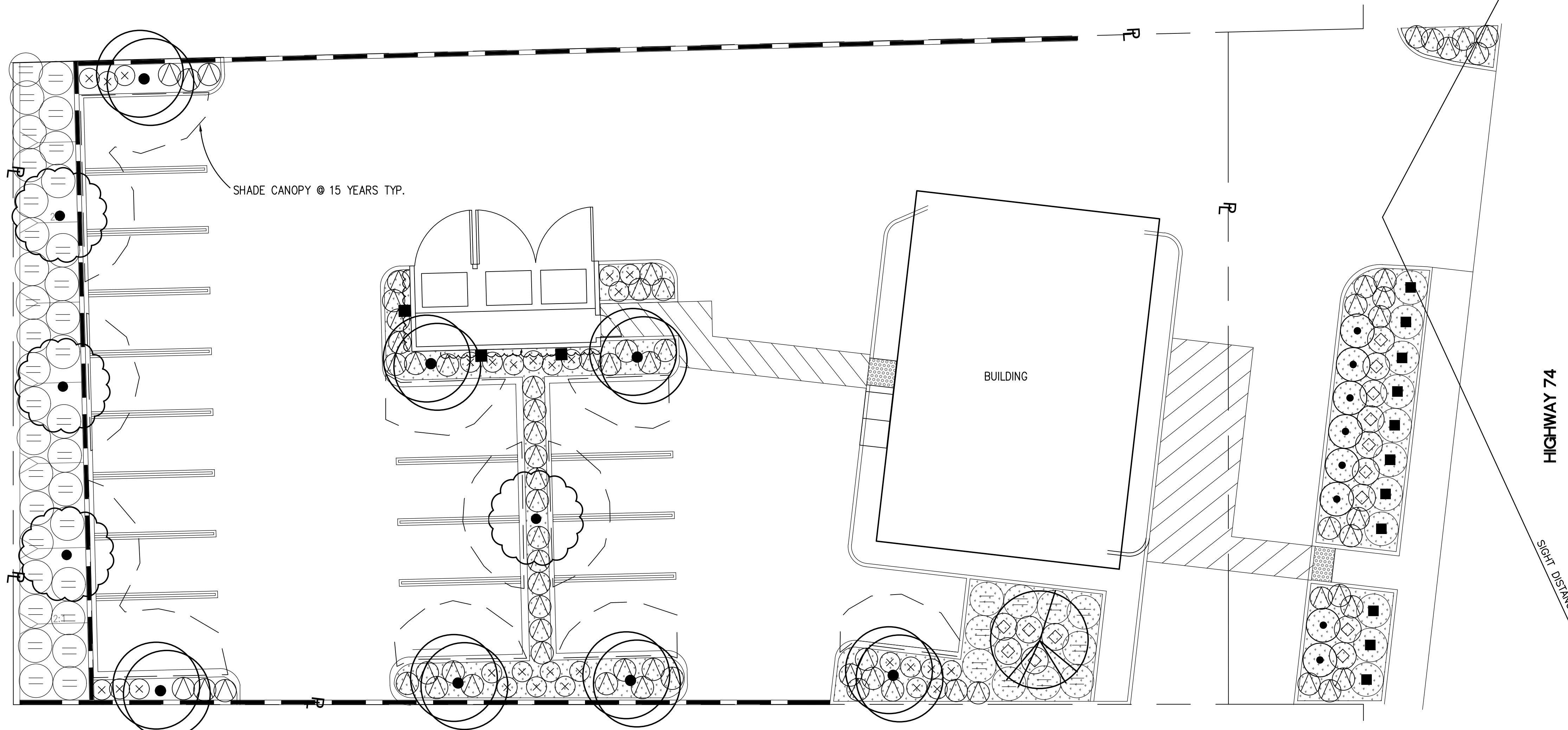
REQUIRED 18 SPACES @ 162 S.F. = 2,916 S.F.
2,916 S.F. @ 10% COVERAGE REQUIRED= 292 S.F.
ACTUAL AREA = 756 S.F.



LANDSCAPE NOTES:

THE FOLLOWING ITEMS WILL BE INCORPORATED INTO THE FINAL LANDSCAPE CONSTRUCTION PLANS AND SPECIFICATIONS.

1. A SMART CONTROLLER W/ AN ET GAGE WITH ACCESS TO REAL-TIME ET (CONTROLLER SHALL BE LIGHT COMMERCIAL RATING MINIMUM)
2. MASTER VALVE AND FLOW SENSOR (EXCEPT FOR PRIVATE RESIDENTIAL LOTS)
3. RAIN SENSING DEVICE
4. ANTI-DRAIN CHECK VALVES
5. PRESSURE REGULATOR (IF NEEDED)
6. HYDROZONES WILL BE PROPERLY DESIGNATED
7. NO OVERHEAD IRRIGATION WITHIN 24" OF NON-PERMEABLE SURFACES. (NO RESTRICTIONS TO METHOD IF ADJACENT TO PERMEABLE SURFACE W/ NO RUNOFF/OVERSPRAY)
8. SUBSURFACE OR LOW-VOLUME IRRIGATION WILL BE USED FOR IRREGULARLY SHAPED AREAS, OR AREAS LESS THAN 10" IN WIDTH. PROVIDE A 3" LAYER OF MULCH (MIN.) IN SHRUB BED AND UNPLANTED AREAS; 2" LAYER OF MULCH IN GROUND COVER AREAS; 3" LAYER OF SHREDDED STABILIZING MULCH FOR SLOPES
10. TREES SHALL BE STAKED W/ 2-3 STAKES AND 6 TIES PER COUNTY STANDARD DETAILS.
11. ROOT BARRIERS SHALL BE INSTALLED FOR TREES WITHIN 6' (MIN.) OF HARDSCAPE PER COUNTY STANDARD DETAILS. ROOT BARRIER SHALL NOT ENCIRCLE THE TREE ROOTBALL BUT SHALL BE LOCATED AT THE EDGE OF HARDSCAPE AND EXTEND BEYOND CENTER OF TREE A MINIMUM OF 5' IN EACH DIRECTION.
12. TREES SHALL HAVE BREATHING TUBES PER COUNTY STANDARD DETAILS.
13. PLANTER ISLANDS ADJACENT TO PARKING SPACES SHALL HAVE A 12" WIDE CONCRETE WALKWAY STRIP INSTALLED ADJACENT TO AND INTEGRAL WITH OR DOWELED INTO THE 6" WIDE CURB.



ASSESSOR'S PARCEL NO.
458-224-010

PROPERTY DESCRIPTION
LOT 11 IN BLOCK B OF VALLEY VISTA ACRES AS SHOWN BY MAP ON FILE IN BOOK 14, PAGE 1 OF MAPS, RIV. CO. RECORDS.

EXCEPTING THEREFROM THE NORTHERLY 20 FEET AS CONVEYED TO THE COUNTY OF RIVERSIDE RECORDED JUNE 19, 1980 AS INST. NO. 112623.

BENCH MARK

RIVERSIDE COUNTY BENCH MARK M-142, A 3.5" ALUM. DISK STAMPED 'RIVERSIDE COUNTY BENCH MARK M-142 1984', FLUSH ON TOP OF THE W/LY END OF A 9" WIDE x 18' LONG CONCRETE DRAINAGE HEADWALL, AT THE W/LY SIDE OF THE ENTRANCE TO THE PANORAMA COVE MOBIL HOME PARK (VINCENT AVE.), 0.9 OF A MILE W/LY ALONG STATE HWY. 74 FROM THE INTERSECTION OF WINCHESTER ROAD (S.H. 79) AND S.H. 74.

ELEV. = 1616.305

PLANTING LEGEND

SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE	NUMBER	REMARKS	WUCOLS IV
TREES:						
☉	CERCIDIMUM 'DESERT MUSEUM'	DESERT MUSEUM PALO VERDE	24" BOX	4	DOUBLE STAKE / HEIGHT 8-10', SPREAD 3'-4' MIN.	L
☉	CHILOPSIS LINEARIS	DESERT WILLOW	24" BOX	1	DOUBLE STAKE / HEIGHT 8-10', SPREAD 3'-4' MIN.	L
☉	CALLISTEMON VIMINALIS	WEeping BOTTLEBRUSH	24" BOX	7	DOUBLE STAKE / HEIGHT 8-10', SPREAD 3'-4' MIN.	L
SHRUBS:						
⊕	DIANELLA 'LITTLE REV'	FLAX LILY	1 GAL	70	FULL & BUSHY @ 3' O.C.	L
⊕	HESPERALOE PARVIFLORA	RED YUCCA	5 GAL	14	FULL & BUSHY @ 4' O.C.	L
⊕	LEUCOPHYLLUM FRUTICOSUM 'GREEN CLOUD'	GREEN CLOUD TEXAS RANGER	5 GAL	8	FULL & BUSHY @ 5' O.C.	L
⊕	LOMANDRA 'BREEZE'	MATT RUSH	1 GAL	157	FULL & BUSHY @ 3' O.C. IN BASIN BOTTOMS	L
⊕	JUNIPERUS HORIZONTALIS 'BLUE RUG'	PROSTRATE COAST ROSEMARY	1 GAL	11	FULL & BUSHY @ 5' O.C.	L
⊕	BACCHARIS PILULARIS 'TWIN PEAKS'	PROSTRATE BLUE JUNIPER	1 GAL	45	TRIANGULAR SPACING @ 5' O.C.	L
⊕	CALLISTEMON VIMINALIS 'LITTLE JOHN'	DWARF BOTTLE BRUSH	5 GAL	28	FULL & BUSHY @ 3' O.C.	L
VINES:						
⊕	MACFADYENA UNGUIS CATI	CAT'S CLAW VINE	5 GAL	3	ATTACH TO WALL	L
MULCH & GROUND COVER:						
NOT SHOWN	FOREST BLEND MULCH	SHREDDED WOOD MULCH	3" MAX.	AS REQ'D.	3" DEEP TO ALL PLANTING AREAS - PLANT'S CHOICE (619) 585-4745	

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE CLEARING OF THE PROPOSED WORK AREA, AND THE RELOCATION COSTS OF ALL UTILITIES. PERMITEE MUST INFORM COUNTY OF CONSTRUCTION SCHEDULE AT LEAST 48 HOURS PRIOR TO BEGINNING CONSTRUCTION. PHONE: (951) 955-6885

I AGREE TO COMPLETE A LANDSCAPE CONSTRUCTION DOCUMENT PACKAGE THAT COMPLIES WITH THE REQUIREMENTS OF ORDINANCE NO. 859.3; ORDINANCE 348, INCLUDING OFF-SITE PARKING AND SHADING REQUIREMENTS; THE CONDITIONS OF APPROVAL; AND IN SUBSTANTIAL CONFORMANCE WITH THE APPROVED SPECIFIC PLAN AND LANDSCAPE CONCEPT PLAN. SHOULD THE ORDINANCES BE REVISED, THESE PLANS MAY BE SUBJECT TO CHANGE BASED ON THE UPDATED ORDINANCES.

Vincent DiDante
APPLICANT'S SIGNATURE

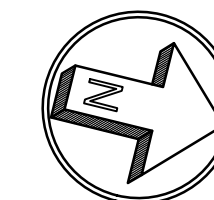
12-13-21
DATE

NOTES:

1. ALL PLANTING AND IRRIGATION SHALL CONFORM TO THE COUNTY OF RIVERSIDE STANDARDS, ORDINANCE 859, THE GUIDE TO CALIFORNIA FRIENDLY LANDSCAPING AND THE TEMESCAL CANYON DESIGN GUIDELINES.
2. AN IRRIGATION SYSTEM SHALL BE UTILIZED FOR THIS PROJECT PER THE COUNTY OF RIVERSIDE'S WATER CONSERVATION ORDINANCE 859.
3. ALL MATURE PLANTING SHALL NOT INTERFERE WITH UTILITY LINES OR TRAFFIC SITE LINES.
4. ALL UTILITIES NOT IN SCE EASEMENT SHALL BE SCREENED W/ PLANTING TYP.
5. INSTALL ROOT BARRIERS TO TREES WITHIN 6' OF ALL HARDSCAPE
6. ALL SLOPES OVER 3' IN VERTICAL HEIGHT SHALL BE PLANTED AND IRRIGATED PER THE BUILDING AND SAFETY REQUIREMENTS.
7. A 3" LAYER OF WOOD MULCH SHALL BE INSTALLED TO ALL PLANTING AREAS AS REQUIRED.
8. ALL LANDSCAPE SHALL BE MAINTAINED BY THE OWNER.
9. ALL LANDSCAPING SHALL BE INSTALLED IN ONE PHASE.

ALL WORK WITHIN STATE RIGHT OF WAY SHALL BE COMPLETED IN ACCORDANCE WITH THE CURRENT 2018 CALTRANS STANDARD PLANS AND SPECIFICATIONS AND 2014 MUTCD. SHOULD A CONFLICT ARISE BETWEEN THE CONTRACT PLANS, SPECIFICATIONS AND/OR DETAILS PREPARED BY THE PERMITEE, THE STATE'S STANDARD SPECIFICATIONS SHALL GOVERN AS DETERMINED BY THE STATE'S REPRESENTATIVE.

CALTRANS PERMIT # 00-00-0-CD-0000

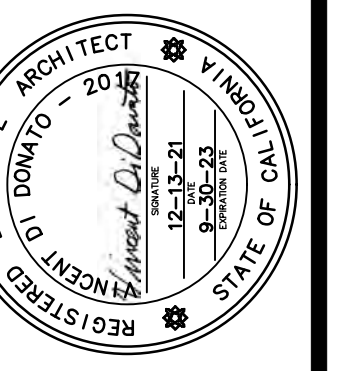
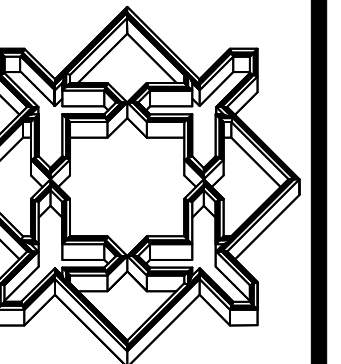


GRAPHIC SCALE



REVISED

ALHAMBRA GROUP
LANDSCAPE ARCHITECTURE
California license #2017
RECREATION FACILITIES PLANNING
41635 Eastspring Circle, North, Suite C
Temecula, CA 92590 (951) 296-6802 FAX: 296-6803



PROJECT: CANNABIS RETAIL FACILITY
OWNER: SHADEN SALAH
33671 STATE HIGHWAY 74, HEMET, CA 92345
PHONE: (951) 956-5162

drawn: V.D.
checked: V.D.
date: 12-10-21

SHEET L-1
of 1 sheets
JOB NO. 21-123

PAGE BREAK



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



June 2, 2022

Ryan Fowler, Principal Planner
City of Menifee Community Development Department
29844 Haun Road
Menifee CA 92586

CHAIR
Steve Manos
Lake Elsinore

VICE CHAIR
Russell Betts
Desert Hot Springs

COMMISSIONERS

Vacant

John Lyon
Riverside

Steven Stewart
Palm Springs

Richard Stewart
Moreno Valley

Michael Geller
Riverside

STAFF

Director
Paul Rull

Simon Housman
Jackie Vega
Barbara Santos

County Administrative Center
4080 Lemon St., 14th Floor.
Riverside, CA 92501
(951) 955-5132

RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW – DIRECTOR'S DETERMINATION

File No.: ZAP1064RG22
Related File No.: LR22-0130 (Development Code Amendment)
APN: Citywide

Dear Mr. Fowler,

As authorized by the Riverside County Airport Land Use Commission (ALUC) pursuant to its Resolution No. 2011-02, as ALUC Director, I have reviewed City of Menifee Development Code Amendment (LR22-0130), a proposal amending its Zoning Code identifying a new use, Battery Energy Storage Systems (BESS), and its associated development standards, definitions and permittance.

The Airport Land Use Compatibility Plan (ALUCP) criteria identifies that the storage of hazardous materials is a prohibited use in Zones B1 and B2. The ALUC has historically identified similar BESS projects as not storing hazardous material, and subsequently found those project's consistent, as long as the project meets local jurisdiction Fire and Building and Safety code regulations. The proposed Development Code Amendment contains development standards for BESS projects requiring all applicable standards of the adopted Building and Safety Code and the adopted Fire Code. With the incorporation of these regulations, it is therefore considered that BESS projects are not considered storage of hazardous materials, and not a prohibited use in the ALUCP. The proposed amendments do not involve changes in development standards or allowable land uses that would increase residential density or non-residential intensity. Therefore, these amendments have no possibility for having an impact on the safety of air navigation within airport influence areas located within the City of Menifee.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan and the 2010/2011 Perris Valley Airport Land Use Compatibility Plan.

This determination of consistency relates to airport compatibility issues and does not necessarily constitute an endorsement of the proposed amendment.

If you have any questions, please contact me at (951) 955-6893.

Sincerely,

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Paul Rull, ALUC Director

cc: ALUC Case File

X:\AIRPORT CASE FILES\Regional\ZAP1064RG22\ZAP1064RG22.LTR.doc

www.rcaluc.org

Exhibit “A”

Article 3: Zones

Chapter 9.135 Commercial and Industrial Zones

Sections:

9.135.030 Allowed Uses and Approval Requirements

- A. **Allowed Use Table.** Table 9.135.030-1 identifies allowed uses and corresponding approval requirements for the commercial and industrial zones subject to compliance with all other provisions of this Title. Descriptions/definitions of many of the land uses can be found in [Article 6](#) of this title (Definitions). The list of land uses on Table 9.135.030 shall be permitted in one or more of commercial and industrial zones as indicated in the columns corresponding to each zone.
- B. **Approval Requirements.** Where indicated with a letter “P” the use shall be a permitted use. A letter “C” indicates the use shall be conditionally permitted subject to the approval of a conditional use permit. Where indicated with a “--,” the use is prohibited within the zone. The Additional Requirements column in the table identifies additional use regulations for specific uses and/or the specific chapter or section where additional regulations for that use type are located within this title, where applicable. Uses for which additional requirements are listed shall be allowed only upon satisfaction of the specified additional requirements. Under no circumstances shall Table 9.135.030-1 authorize a use without satisfaction of such specified additional requirements
- C. **Unlisted Uses.** Uses not specifically listed in this table shall be considered not permitted in all of the listed zones. The Community Development Director may make a determination pursuant to [Chapter 9.95](#) (Similar Use Determination) that new uses not listed here that have substantially similar characteristics to specific listed permitted uses may be considered permitted as the similar use by right, or conditionally as appropriate.

D.

Table 9.135.030-1 Commercial and Industrial Zones – Allowed Uses and Approval Requirements					
	CR	CO	BP	HI	
Adult businesses	--	--	--	--	See Ordinance XX
Airport	--	--	--	C	
Ambulance services	C	C	P	P	
Amusement arcade	P	--	C	--	
Amusement park, (including multiple activities such as simulated flying, racing, mini-golf, etc.)	P		P		
Indoor	C		C		
Outdoor					
<u>Animals</u> , Small (e.g. hamsters, rabbits, chinchillas, and similar sized <u>animals</u>)					Chapter 9.235 (Animal Keeping)
Non-Commercial	P	P	P	P	
Commercial	--	--	--	--	
<u>Animal hospitals</u> and veterinary services (with outdoor facilities)	P	--	P	C	
	C		C	--	
<u>Animal Rescue</u>	--	--	C	C	Chapter 9.235 (Animal Keeping)
Antique shops, pawn shops, thrift stores	P	--	C	--	Outside collection bins prohibited
Art gallery, library, reading room, <u>museum</u> .	P	P	--	--	
Art supply shops and studios.	P	P	P	--	
<u>Auction</u> Houses	P	--	P	--	
Auditoriums, event centers and assembly areas, including <u>live entertainment</u> :					
Indoor	P	C	P	--	
Outdoor	C	--	C	--	
Bakery goods distributors.	P	--	P	P	
Bakery shops, Coffeehouse, Cybercafé, Delicatessens, Ice cream shops	P	P	P	--	
Banks and financial institutions.	P	P	P	--	
Banquet facilities.	C	--	C	--	
Batting cages – indoor	P	--	C	--	
Outdoor	C		C		
Billiard and pool halls, Bowling <u>alleys</u>	P	--	P	--	Chapter 9.250 (Alcohol Sales)
Breweries, distilleries, and wine making facilities with on-site tasting room and sales for <u>off-site</u> consumption	C	--	P	--	Chapter 9.250 (Alcohol Sales)
<u>Business services</u>	P	P	P	--	
Car washes	P	--	C		
<u>Caretaker residence</u> or on-site operator residence, only as accessory to <u>primary use</u>	C	C	C	C	
Catering services	P	--	P	--	
Cemeteries and mausoleums	--	--	C	C	
<u>Clinics</u> , including but not limited to medical and urgent care	C	C	C	--	
<u>Collection Containers</u>	--	--	--	--	
Commercial television and radio broadcast <u>structures</u>	--	--	--	P	
Concrete batch plants and asphalt plants	--	--	--	C	
<u>Congregate care facility</u>	--	C	C	--	Chapter 9.270 (Community)

Table 9.135.030-1 Commercial and Industrial Zones – Allowed Uses and Approval Requirements					
	CR	CO	BP	HI	
					Care Facilities)
Contractor, landscape and building materials storage yard	--	--	C	C	
Convenience stores, not including the sale of motor vehicle fuel	P	--	P	--	
Dance Halls, night clubs, discos, cabarets, cocktail lounges, lodges and incidental dancing areas, and similar facilities where dancing is the principal use	C	--	C	--	Chapter 9.250 (Alcohol Sales)
Day Care Center	P	C	C	--	Chapter 9.270 (Community Care Facilities)
Department stores	P	--	P	--	
Drug Store	P	P	--	--	
Dry cleaning and laundromats (except uniform supply and industrial laundrers);	P	C	C	--	
Educational Institution	C	C	C	C	
Emergency shelters	--	--	--	P	In locations specified in the Housing Element
Battery Energy Storage Facility, Utility Scale	--	--	C	C	Chapter 9.300 (Energy Storage Facilities)
Equipment sales and rental, Large (including large vehicles, trucks with beds over 18 feet in length, eighteen plus (18+) wheelers, and construction equipment)	--	--	--	C	
Equipment sales and rental, Small (including rototillers, power mowers, sanders, power saws, cement and plaster mixers not exceeding 20 cubic feet in capacity and other similar equipment)	P	--	C	C	
Fast food/quick service, with drive-through	C	C	C	--	
Fast food/quick service, without drive-through	P	P	P	--	
Feed and grain sales	C	--	--	P	
Fertilizer production and processing (organic or inorganic)	--	--	--	C	
Funeral parlor, mortuary with crematorium	--	C	C	--	
Funeral parlor, mortuary without crematorium	P	C	C	--	
Gas station with/without convenience store, with or without alcoholic beverage sales, and with or without car wash	C	--	C	C	
General retail	P	--	--	--	
General warehousing, distribution centers, and storage (except noxious, explosives, or dangerous materials)	--	--	P	P	
Golf Course	--	--	--	--	
Golf driving range (not in association with full scale course)	C	--	C	--	
Governmental facility	P	P	P	P	
Grocery Store	P	--	--	--	Chapter 9.250 (Alcohol Sales)
Hardware and Building Supplies without outdoor sales/storage	P	--	P	P	
with outdoor sales/storage	C	--	C	C	
Health, fitness, dance, martial arts studio <5,000 sq.ft. >5,000 sq.ft.	P C	C C	P C	--	
Heliports	--	--	C	C	
Hospital	C	C	--	--	

Table 9.135.030-1 Commercial and Industrial Zones – Allowed Uses and Approval Requirements					
	CR	CO	BP	HI	
<u>Hotels and resort hotels</u>	P	C	C	--	
<u>Kennel or cattery</u>	--	--	C	C	Chapter 9.235 (Animal Keeping)
<u>Laboratories, research and development</u>	P	P	P	--	
<u>Live/work units</u>	C	C	C	--	
<u>Lumber yard</u>	--	--	--	C	
<u>Manufacturing, Handcraft</u>	P	P	P	P	
<u>Manufacturing, Light-Intensity</u>	--	C	P	P	
<u>Manufacturing, Medium-Intensity</u>	--	C	C	P	
<u>Manufacturing, Heavy-Intensity</u>	--	--	--	P	
<u>Marijuana dispensaries, mobile marijuana dispensaries, marijuana cultivation, and marijuana processing</u>	--	--	--	--	
<u>Massage Establishment</u>	C	C	--	--	
<u>Material storage yard</u>	--	--	C	P	
<u>Membership clubs, organizations, and lounges</u>	C	C	C	--	
<u>Mobile home sales and storage, trailer sales and rental house trailers</u>	C	--	C	C	
<u>Motels</u>	C	--	--	--	
<u>Motocross Facilities, Bicycle (BMX) course</u>	--	--	--	--	
<u>Motor vehicle body, paint and upholstery shops</u>	C	--	C	P	Chapter 9.240 (Motor Vehicle and Related Uses)
<u>Motor vehicle parts and supply stores</u>	P	--	C	--	
<u>Motor vehicle repair/services (e.g. tune-ups, emission tests, brakes, tires, batteries, electrical)</u>	P	--	C	P	Chapter 9.240 (Motor Vehicle and Related Uses)
<u>Motor vehicle repairs/services - major (e.g., engine and transmission repair/rebuild, etc.)</u>	--	--	C	P	Chapter 9.240 (Motor Vehicle and Related Uses)
<u>Motor vehicle sales and rental (including outdoor display area and repairs associated with sales)</u>	C	--	C	--	Chapter 9.240 (Motor Vehicle and Related Uses)
<u>Motor vehicle wholesale, with no outdoor display</u>	P	P	P	P	
<u>Motor vehicle impound, wrecking and junk yards</u>	--	--	--	C	Chapter 9.240 (Motor Vehicle and Related Uses)
<u>Motor vehicle, trailer or boat storage</u>	--	--	C (indoor only)	C	Chapter 9.240 (Motor Vehicle and Related Uses)
<u>Nurseries and garden supply, indoor and outdoor (retail sales only)</u>	P	--	--	P	Does not include cultivation of marijuana/cannabis.
<u>Offices, professional and medical</u>	P	P	P	--	
<u>Parking lots and parking structures</u>	P	P	P	P	
<u>Personal and Professional Services</u>	P	--	P	--	
<u>Public utility substations and storage buildings</u>	--	--	--	P	
<u>Radio and television broadcasting and recording studios</u>	P	--	P	--	
<u>Recreational vehicle, trailer and mobile home sales and rental.</u>	C	--	C	C	Chapter 9.240 (Motor Vehicle and Related Uses)
<u>Recycling collection facilities</u>	C	--	C	C	Chapter

Table 9.135.030-1 Commercial and Industrial Zones – Allowed Uses and Approval Requirements					
	CR	CO	BP	HI	
					9.280 (Recycling Facilities)
<u>Recycling processing facilities</u>	--	--	C	C	Chapter 9.280 (Recycling Facilities)
Religious institutions	P	P	P	P	
<u>Restaurant</u>	P	P	P	P	Chapter 9.250 (Alcohol sales)
<u>Residential care facility</u>	--	C	--	--	Chapter 9.270 (Community Care Facilities)
<u>Restaurants</u> with breweries, distilleries and/or wine making facilities with sales for on-site and <u>off-site</u> consumption	C	C	C	C	Chapter 9.250 (Alcohol sales)
<u>Self-Storage</u> , public storage facilities	C	--	C	P	
<u>Shooting range</u> ; Indoor	--	--	C	--	
<u>Shooting range</u> ; Outdoor	--	--	--	--	
Simulated shooting games, Indoor (laser tag, etc.)	C	--	C	--	
Simulated shooting games, Outdoor (paintball, etc.)	--	--	--	--	
Solid Waste Disposal	--	--	--	C	
<u>Specialized Retail</u>	P	--	P	--	
Sports and recreational facilities (not including motor driven vehicles and riding academies, but including archery ranges, athletic playgrounds, athletic fields, sports arenas, skating rinks, skate <u>parks</u> stadiums, and commercial swimming pools)					
Indoor	P	--	C	--	
Outdoor	C	--	C	--	
Studios for professional work in or teaching of any form of fine arts	P	C	P	--	
<u>Swap Meets</u> (Indoor only)	C	--	--	C	All activities must be conducted indoors.
Tattoo Establishments	C	C	C	--	Chapter 9.265 (Tattoo Establishments)
Theater	C	--	C	--	
Tourist information centers	P	P	P	--	
Towing services (with tow truck parking - no auto storage)	--	--	P	P	
Towing services (with tow truck parking and auto storage)	--	--	--	C	
<u>Transportation Stations</u> (bus, railroad and taxi)	P	P	P	P	
Trucking and freighting operations	--	--	--	C	
Uniform supplier and <u>industrial</u> launderers	--	--	C	P	
<u>Utility</u> offices, uses, and <u>structures</u>	P	P	P	P	
Vehicle storage and impoundment within an enclosed <u>building</u>	--	--	P	P	
Vocational/trade <u>school</u> ;	C	--	P	C	
<u>Warehouse</u> /club store, Standalone facility 50,000 sq. ft. or larger	C	--	C	--	
Wholesale businesses with samples on the <u>premises</u> but not including storage	P	--	P	--	
Wholesale stores and distributors	--	--	C	--	
<u>Wireless Communication Facilities</u>	C	C	C	C	Chapter 9.290 (Wireless Communications)

Table 9.135.030-1 Commercial and Industrial Zones – Allowed Uses and Approval Requirements					
	CR	CO	BP	HI	
					Facilities)

Article 5: Special Use Standards

Chapter 9.300 Energy Storage Facilities

Contents:

9.300.010 Purpose

9.300.020 Classification of Energy Storage Facilities

9.300.030 Applicability

9.300.040 Development Standards

9.300.050 Decommissioning

9.300.060 Performance Measures and Standard Conditions of Approval

9.300.010 Purpose

Energy Storage Facility regulations are adopted with the intent of advancing and protecting the public health, safety, and welfare of the City of Menifee by establishing regulations for the installation and use of energy storage systems. The regulation herein are intended to protect the health, welfare, safety, and quality of life for the general public, to ensure compatible land uses in the areas affected by energy storage facilities and to mitigate the impacts of energy storage facilities on the environment.

9.300.020 Classification of Energy Storage Facilities

The following words and phrases shall, for the purposes of this chapter, have the meanings respectively ascribed to them by this section, as follows:

1. Battery: A single cell, stack, core building block, or a group of cells connected together electrically in series, in parallel, or a combination of both, which can charge, discharge, and store energy electrochemically. For the purposes of this chapter, batteries utilized in consumer products are excluded from these requirements.
2. Battery Management System: An electronic system that prevents storage batteries from operating outside their safe operating parameters and disconnects electrical power to the energy storage system or places it in a safe condition if potentially hazardous temperatures or other conditions are detected. The system generates an alarm and trouble signal for abnormal conditions
3. Battery Energy Storage System: A system consisting of electrochemical, kinetic, thermal, or other form of energy-storage technology storage batteries, battery chargers, controls, power conditioning systems and associated electrical equipment, assembled together, capable of storing energy in order to supply electrical energy at a future time, not to include a stand-alone 12-volt car battery or an electric motor vehicle.
4. Cell: The basic electrochemical unit, characterized by an anode and a cathode, used to receive, store, and deliver electrical energy.
5. Commissioning: A systematic process that provides documented confirmation that a battery energy storage system functions according to the intended design criteria and complies with applicable code requirements.

6. Decommissioning Plan: A plan to retire the physical facilities of the Project, including decontamination, dismantlement, rehabilitation, landscaping and monitoring. The plan contains detailed information on the proposed decommissioning and covers the schedule, type and sequence of decommissioning activities; waste management, storage and disposal of the waste from decommissioning; the timeframe for decommissioning and site rehabilitation.

7. Energy Storage System: A system which stores energy and releases it in the same form as was input.

9.300.030 Applicability

The requirements of this ordinance shall apply to all utility-scale battery energy storage facilities permitted, installed, or modified after the effective date of this ordinance, excluding general maintenance and repair. Utility-scale battery energy storage systems constructed or installed prior to the effective date of this ordinance shall not be required to meet the requirements of this chapter. Modifications to, retrofits or replacements of an existing battery energy storage facility that increases the total energy storage system designed discharge duration or power rating shall be subject to this chapter. The continuation of legally established existing utility-scale battery energy storage systems shall be subject to the regulations and guidelines of Chapter 9.15 Nonconforming Uses and Structures of the City of Menifee Municipal Code.

9.300.040 Development Standards

1. Energy storage facilities must meet all applicable standards of the adopted Building and Safety Codes and of the adopted Fire Codes.

2. Energy storage facilities shall comply with the site design requirements set forth below in addition to all other applicable chapters of the City of Menifee Municipal Code:

a. Where visible from public view, the site shall be fully enclosed by a minimum six-foot, non-scalable solid wall. Where not visible from public view, the site shall be enclosed by a minimum six-foot, non-scalable solid wall or tubular steel or wrought iron fencing. Said wall or fencing shall be located outside of the required yard of the applicable zone. Walls shall consist of either decorative concrete masonry block or decorative concrete tilt-up walls. Decorative masonry block means neutral colored slump stone block, split-face block, or precision block with a stucco, plaster, or cultured stone finish. Decorative concrete tilt-up wall means concrete with a combination of paint and raised patterns, reveals, and/or trim lines.

When sound and visual attenuation requires a wall exceeding 6 feet above the grade of the adjacent roadway, earth mounds shall be used, such that no more than 6 feet of the wall is visible from the roadway. The mounds shall not exceed a three-to-one (3:1) ratio slope. The mounds may support the wall or be placed against the wall on the street side.

Maximum wall heights shall comply with Section 9.185.040(g) ("Maximum Heights") of this Title.

b. No landscaping is required on the interior side of the screen walls described in subparagraph (a), above, regardless of the percentage of open space landscaped required.

The minimum required open space, as required by this Title, shall be limited to the perimeter landscaping surrounding the perimeter screening wall or fencing described in subparagraph (a), above. Said perimeter landscaping shall be no less than the applicable required yard setbacks.

c. Solid walls surrounding facilities which are below grade of an adjacent street or property shall incorporate a berm/slope along the entire length of the wall to ensure facilities are not visible from public view.

d. Anti-graffiti coating or equivalent measure to prevent graffiti shall be provided for all solid screen walls.

e. Except as set forth in subparagraph (f), no equipment or appurtenances not in an enclosed structure shall exceed the screen wall height described in subparagraph (a), above, unless it can be demonstrated through a line-of-sight analysis to the satisfaction of the approval body that the wall height will sufficiently screen said equipment and/or appurtenances. Enclosures for batteries and other systems shall not exceed fifteen feet in height.

f. Accessory structures such as utility poles or utility connection equipment, substation switchyard and similar equipment, necessary for the operation of the facility may exceed the height standards of the applicable zone subject to Planning Commission approval.

g. Within the Business Park (BP) zone, all equipment or appurtenances shall be located within an enclosed building. However, accessory structures such as utility poles or utility connection equipment substation switchyard, and similar equipment, necessary for the operation of the facility is not required to be located within the enclosed building.

h. On-site parking shall be provided as specified below:

i. For sites occupied daily by employees or contractors, one parking space per employee or contractor shall be provided.

ii. For unoccupied sites, one on-site parking space shall be provided.

iii. All parking, fire access roadway, and drive aisles shall be paved with asphalt or concrete.

iv. All parking lots shall comply with Section 9.215, Parking and Loading Standards.

vi. All facilities shall have an approved signage plan including safety signage to be posted at the site.

i. All improvements and site specifications shall be subject to approval of a Hazard Mitigation Analysis. Said reports shall be reviewed by the City of Menifee Fire Department and Building and Safety Departments. These reports must be approved concurrent with any entitlements. The purpose of this analysis is to evaluate the potential for adverse effects to people or the environment related to hazards and hazardous materials. The California Environmental Quality Act (CEQA) requires the analysis of potential adverse effects of a project on the environment. Consistent with Appendix G of the CEQA Guidelines, a proposed project would cause adverse impacts related to hazards and hazardous materials if they would create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment.

9.300.050 Decommissioning

1. Decommissioning Plan. Prior to issuance of any permits related to decommissioning activities, the applicant shall submit a decommissioning plan containing a narrative description of the activities to be accomplished for removing the energy storage system from service, and from the facility in which it is located. The decommissioning plan shall also include:

a. A narrative description of the activities to be accomplished, including who will perform that activity and at what point in time, for complete physical removal of all battery energy storage system components, structures, equipment, security barriers, and transmission lines from the site;

b. Disposal of all solid and hazardous waste in accordance with local, state, and federal waste disposal regulations;

c. The anticipated life of the battery energy storage system;

d. The manner in which the battery energy storage system will be decommissioned, and the Site restored, including a description of how any changes to the surrounding areas and other systems adjacent to the battery energy storage system, such as, but not limited to, structural elements, building penetrations, means of egress, and required fire detection suppression systems, will be protected during decommissioning and confirmed as being acceptable after the system is removed; and

e. A listing of any contingencies for removing an intact operational energy storage system from service, and for removing an energy storage system from service that has been damaged by a fire or other event.

2. Ownership Changes. If the owner of the battery energy storage facility changes or the owner of the property changes, the project approvals shall remain in effect, provided that the successor owner or operator assumes in writing all the obligations of the project, site plan approval, and decommissioning plan. A new owner or operator of the battery energy storage facility shall notify the Community Development Department of such change in ownership or operator within 30 days of the ownership change. A new owner or operator must provide such notification to the Community Development Department in writing. The project and all approvals for the battery energy storage facilities would be void if a new owner or operator fails to provide written notification to the Community Development Department in the required

timeframe. Reinstatement of a voided project or approvals will be subject to the same review and approval processes for new applications under this chapter.

9.300.060 Performance Measures and Standard Conditions of Approval

The following measures shall be included as performance measures and standard conditions of approval for all energy storage facilities.

- a. Facilities shall not store any products, goods, materials, or containers outside of any building on-site.
- b. Facilities shall comply with Chapter 9.210 Noise Control Regulations of the City of Menifee Municipal Code.
- c. Operators shall address any nuisance, safety issues or violations of conditions of approval within forty-eight hours of being notified by the city that an issue exists.
- d. Prior to the issuance of a Certificate of Occupancy or Business License, any operator of an energy storage facility shall sign a statement acknowledging acceptance of all operational conditions of approval associated with the approved entitlements for the facility.

Article 6: Definitions

Chapter 9.300 Universal Definitions

Sections:

9.300.030 “B” Definitions

Balcony. A platform that projects from the wall of a building, typically above the first level, and is surrounded by a rail, balustrade or parapet.

Bar. Premises used primarily for the dispensing of alcoholic beverages by the drink for on-site consumption.

Battery: A single cell, stack, core building block, or a group of cells connected together electrically in series, in parallel, or a combination of both, which can charge, discharge, and store energy electrochemically. For the purposes of this chapter, batteries utilized in consumer products are excluded from these requirements.

Battery Management System: An electronic system that prevents storage batteries from operating outside their safe operating parameters and disconnects electrical power to the energy storage system or places it in a safe condition if potentially hazardous temperatures or other conditions are detected. The system generates an alarm and trouble signal for abnormal conditions

Battery Energy Storage Systems. A system consisting of electrochemical storage batteries, battery chargers, controls, power conditioning systems and associated electrical equipment, assembled together, capable of storing energy in order to supply electrical energy at a future time, not to include a stand-alone 12-volt car battery or an electric motor vehicle.

Battery Energy Storage Facility, Utility Scale. Stationary batteries that are connected to distribution/transmission networks or power-generation assets. Utility scale may also be referred to as “front-of-the-meter”, large-scale or grid-scale battery storage. Utility Scale systems are intended primarily to interact with the electric grid and are not intended to serve a specific end user “behind-the-meter”. Utility-scale systems increase flexibility in power systems, provide grid reliability support and enable an optimal use of variable electricity sources like photovoltaic and wind.

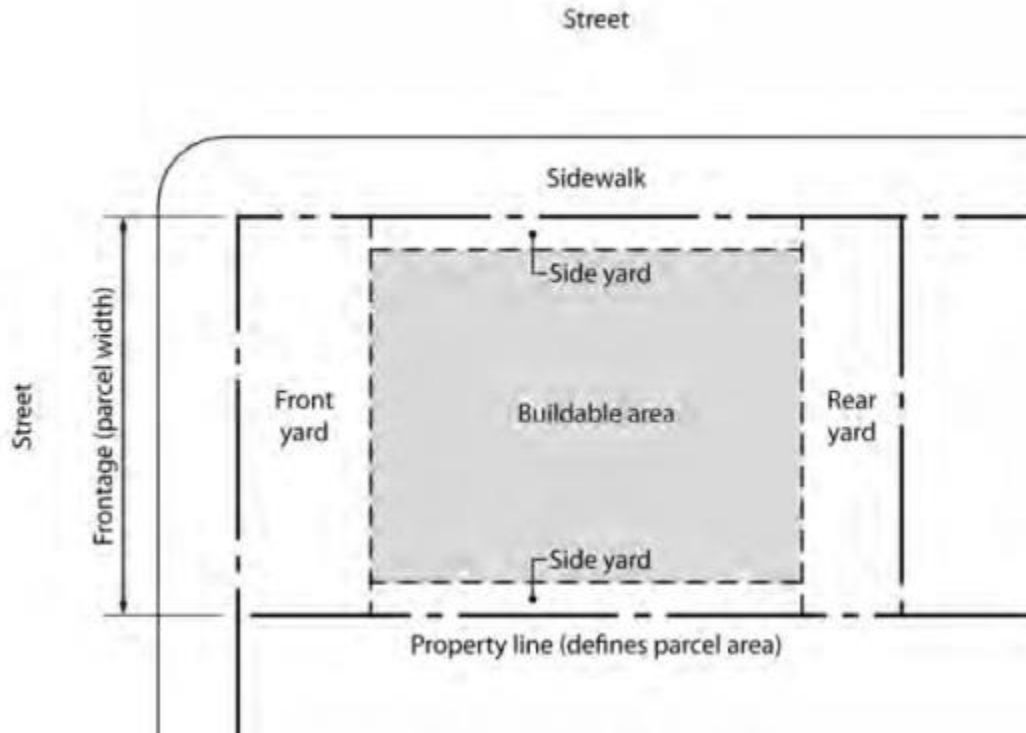
Bed and Breakfast. A business operated in an owner-occupied residence offering short-term rental of overnight accommodations, which provides on-site breakfast service.

Bedroom. A private room for sleeping, separated from other rooms, and accessible to a bathroom without crossing another bedroom.

Berm. A mound of earth or the act of pushing earth into a mound.

Buffer Area. A strip of land established to protect one type of land use from another land use. A buffer may include a physical separation in the form of a specified distance, landscaping, berms or other screens which block vision, noise or other negative impacts.

Buildable Area/Building Envelope. The area of a lot remaining after the minimum setback and open space requirements have been met; the area within which primary and accessory structures may be located, unless an encroachment into the setback is otherwise permitted.



Building. Any structure having a roof, or fully enclosed by walls, and intended for the shelter, housing or enclosure of persons, animals or property.

Building, Accessory. See *Structure, Accessory*.

Building Height. Building height shall be measured in accordance with Section in [9.195.040.A](#) (Height Measurement).

Building Permit. Written permission issued by the City for the construction, repair, alteration or addition to a structure or building.

Building, Primary. A building in which the principal use is conducted.

Business Services. Rendering services to business establishments on a fee or contract basis, including printing and copying, blueprint services, advertising and mailing, office-related equipment rental and leasing, commercial research, development and testing, photo finishing and model building.

Sections:

9.300.050 "D" Definitions

Day Care Centers. Establishments providing nonmedical care for persons on a less than 24-hour basis other than in a family day care home (see *Family Day Care Home*). This classification includes nursery schools, preschools, and day-care facilities for children or adults, and any other day-care facility licensed by the State of California.

Days. Shall always be consecutive calendar days, unless otherwise stated.

Decommissioning Plan: A plan to retire the physical facilities of the Project, including decontamination, dismantlement, rehabilitation, landscaping and monitoring. The plan contains

detailed information on the proposed decommissioning and covers the schedule, type and sequence of decommissioning activities; waste management, storage and disposal of the waste from decommissioning; the timeframe for decommissioning and site rehabilitation.

Dedication. The transfer of property, such as streets and roads or other public usage, to a public agency or utility for specific purposes.

Density. The number of dwelling units, households or housing structures per unit of land.

Density Transfer. The transfer of all or part of the permitted density from one parcel to another parcel.

Detached. Any building or structure that does not have a wall or roof in common with any other building or structure.

Developer. The legal or beneficial owner or owners of property to be developed or being developed, including the holder of an option or contract to purchase or other person(s) having enforceable proprietary interests in such property.

Development. Any man-made change to improved or unimproved real estate, including but not limited to buildings or other structures, mining, dredging, filling, grading, paving, excavation or drilling operations, or storage of equipment or materials.

Development Agreement. A contract between the City and a developer through which the developer receives vested rights to construct a project subject to specific requirements benefiting the community.

Development Code. A set of land use regulations, as contained in Title 9 of the Menifee Municipal Code, adopted by the City which prescribes standards and regulations for land use and development.

Development Rights. The legally established right to develop land by a party.

Director. The Community Development Director of the City of Menifee or his/her designee.

Discretionary Decision. An action taken by a governmental agency that calls for the exercise of judgment in deciding whether to approve and/or how to carry out a project.

District. A zone or geographic area in the municipality within which certain zoning or development regulations apply.

Drainage. Surface water runoff; the removal of surface water or groundwater from land by drains, grading or other means, which include runoff controls to minimize erosion and sedimentation during and after construction or development.

Drainage Area. A geographical area, formed by topography, which collects and directs surface runoff from precipitation to natural or man-made channels.

Drive-through. Any portion of a building or development intended to allow service direct from the building through a window, kiosk or automated delivery system to vehicle occupants. Such facilities include but are not limited to food service windows, automatic teller machines or similar service systems.

Driveway. A roadway providing direct access for vehicles between a street or highway and an area containing parking spaces, loading, storage or refuse collection areas.

Dwelling. A structure or portion thereof which is used for human habitation, including provision for living, sleeping, eating, cooking and sanitation.

Single Family, Attached. A dwelling unit designed for occupancy by one household, located on a single lot and typically grouped together with similar units. They may be attached through vertical party wall(s) to one or more dwellings on abutting lots or may be joined by carports or garages.

Single Family, Detached. A dwelling unit designed for occupancy by one household and located on a separate lot from any other dwelling, except permitted accessory dwelling units. This classification includes individual manufactured housing units installed on a foundation system pursuant to Section 18551 of the California Health and Safety Code

Duplex. A single building on a lot that contains two dwelling units or two single-unit dwellings on the same lot. Duplex does not include a single family dwelling with an accessory dwelling unit on the same lot, which is an accessory residential unit as defined by State law and this Title (see Accessory Dwelling Unit).

Multiple Family. A single building on a lot that contains three or more dwelling units or three or more single-unit dwellings on the same lot, or any combination thereof.

Menifee North Specific Plan No. 260, Amendment 3

ORDINANCE NO. 348.4581

AN ORDINANCE OF THE COUNTY OF RIVERSIDE AMENDING ORDINANCE NO. 348 RELATING TO ZONING

The Board of Supervisors of the County of Riverside ordains as follows:

Section 1. Article XVIIa, Section 17. 73 of Ordinance No. 348 is amended in its entirety to read as follows:

SECTION 17.73 S.P. ZONE REQUIREMENTS AND STANDARDS FOR SPECIFIC PLAN NO. 260.

- a. Planning Areas 2 and 3.
 1. The uses permitted in Planning Areas 2 and 3 of Specific Plan No. 260 shall be the same as those uses permitted in Article XII, Section 12.2 of Ordinance No. 348, in addition Utility-Scale Battery Energy Storage Systems are permitted subject to Chapter 9.300 of the City of Menifee Municipal Code.
 2. The development standards for Planning Areas 2 and 3 of Specific Plan No. 260 shall be the same as those standards identified in Article XII, Section 12.4 of Ordinance No. 348. However, the development standards of Chapter 9.300 shall apply to Utility-Scale Battery Energy Storage System uses.
 3. Except as provided above, all other zoning requirements shall be the same as those requirements identified in Article XII of Ordinance No. 348.



**AIRPORT LAND USE COMMISSION MEETING
MINUTES
May 12, 2022**

DRAFT

5-19-22

COMMISSIONERS PRESENT: Russell Betts, Michael Geller, Steve Manos, Steven Stewart, Larry Froehlich (alternate for Richard Stewart), Beth LaRock (alternate for John Lyon)

COMMISSIONERS ABSENT: Richard Stewart, John Lyon

2.0 PUBLIC HEARING: CONTINUED ITEMS
None

3.0 PUBLIC HEARING: NEW CASES

3.1 Staff report recommended: **ZAP1086BD22 – AT&T (Representative: Smartlink Group)** – City of Palm Desert Planning Department Case No. CUP22-0001 (Conditional Use Permit). A proposal to establish a 65-foot-tall mono-palm tree wireless communications facility with a 562 square foot equipment shelter on 0.81 acres, located at 39750 Garand Lane (Airport Compatibility Zone C of the Bermuda Dunes Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

CONDITIONALLY CONSISTENT

Staff recommended at hearing: **CONDITIONALLY CONSISTENT**

ALUC Commission Action: **CONDITIONALLY CONSISTENT, subject to the conditions included herein, and such additional conditions as may be required by the FAA OES.**

**Motion: Michael Geller
Second: Larry Froehlich**

3.2 Staff report recommended: **ZAP1117FV22 – HD Commercial Ventures, Inc. (Representative: Strom Entitlement Permitting PM, LLC)** – County of Riverside Case No. PPT210140 (Plot Plan). A proposal to construct two industrial buildings totaling 173,653 square feet with mezzanines on 11.30 acres, located northerly of Murrieta Hot Springs Road, southerly of Technology Drive, westerly of Sky Canyon Drive, and easterly of Winchester Road. (Airport Compatibility Zone C of the French Valley Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

CONSISTENT

Staff recommended at hearing: **CONSISTENT**

ALUC Commission Action: **CONSISTENT (Vote 5-1; Steven Stewart dissenting)**

**Motion: Russell Betts
Second: Michael Geller**

VIDEO:

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**AIRPORT LAND USE COMMISSION MEETING
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May 12, 2022**

- 3.3 Staff report recommended: **ZAP1509MA22 – Black & Veatch** – March Joint Powers Authority Case No. COM-Solar 22-003 (Building Permit). A proposal to construct an 84,000 square foot rooftop solar panel system on an existing industrial building on 45 acres, located northerly of Van Buren Boulevard, westerly of Opportunity Way, and easterly of Meridian Parkway (Airport Compatibility Zone B2 of the March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org
- CONSISTENT**
- Staff recommended at hearing: **CONSISTENT**
- ALUC Commission Action: **CONSISTENT (Vote 6-0)**
- Motion: Russell Betts
Second: Steve Manos**
- 3.4 Staff report recommended: **ZAP1516MA22 – Optimus Building Corporation (Representative: Mike Naggar & Associates)** – City of Perris Case Nos. SPA22-05047 (Specific Plan Amendment), DPR22-00006 (Development Plan Review). A proposal to construct an 878,750 square foot industrial manufacturing building with mezzanines on a 40.75 acres located northerly of Ramona Expressway, westerly of Redlands Avenue, easterly of Perris Boulevard, and southerly of Perry Street (approximately 4.5 acres of the site is planned for future commercial development – there are no entitlements proposed for this area at this time). The applicant also proposes amending the Perris Valley Commerce Center Specific Plan rezoning the site from Commercial to Light Industrial (Airport Compatibility Zones C1 and D of the March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org
- CONSISTENT**
- Staff recommended at hearing: **CONSISTENT**
- ALUC Commission Action: **CONSISTENT (Vote 6-0)**
- Motion: Michael Geller
Second: Larry Froehlich**
- 3.5 Staff report recommended: **ZAP1515MA22 – Meridian Park West, LLC (Representative: Waypoint Property Group)** – March Joint Powers Authority Case Nos. GP 21-01 (General Plan Amendment), SP21-01 (Specific Plan), PP21-03 and PP21-04 (Plot Plans), TTM38063 (Tentative Tract Map). The applicant proposes the West Campus Upper Plateau Specific Plan, encompassing 817.9 acres within multiple Airport Compatibility Zones located southerly of Alessandro Boulevard, westerly of Meridian Parkway, northerly of Grover Community Drive, and easterly of Trautwein Road. As part of this project, the applicant proposes amending the General Plan land uses to increase Parks/Recreation and Open Space from 122 acres to 453 acres, eliminate approximately 622.5 acres of Business Park, eliminate approximately 63 acres of Industrial property, approve a 2.6 acre Public Facility area for an existing water tank, and adopt the West Campus Upper Plateau Specific Plan (SP-) on approximately 351 acres and creating policies for the future recordation of a 445 acre Conservation Area. The applicant also proposes to adopt Specific Plan SP-9 containing development standards, design guidelines, infrastructure master plans, maintenance responsibilities, phasing schedule, and implementation procedures necessary to develop a 359 acre business park and adjacent park space. The Specific Plan proposes 43.1 acres of Mixed Use, 66.4 acres of Business Park, 143.3 acres of Industrial, 28.9 acres of streets and roadways, 10 acres of developed
- CONSISTENT (GPA, Specific Plan); CONDITIONALLY CONSISTENT (Plot Plans, Tract Map)**
- Staff recommended at hearing: **CONSISTENT (GPA, Specific Plan, Plot Plans, Tract Map) subject to the conditions included herein, and added conditions from the FAA OES, and added condition that in the event the future BASH study, as prepared by a qualified wildlife hazard biologist, raises significant issues, that the study comes back to the ALUC for review.**
- ALUC Commission Action: **CONSISTENT (GPA, Specific Plan, Plot Plans, Tract Map) subject to the conditions**

VIDEO:

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**AIRPORT LAND USE COMMISSION MEETING
MINUTES
May 12, 2022**

included herein, and added conditions from the FAA OES, and added condition that in the event the future BASH study, as prepared by a qualified wildlife hazard biologist, raises significant issues, that the study comes back to the ALUC for review. (Vote 6-0)

Motion: Michael Geller
Second: Steven Stewart

Parks/Recreation/Open Space, 64.5 acres of undeveloped Parks/Recreation/Open Space, and 3.5 acres of Public Facilities. The Specific Plan will adopt zoning on the properties consistent with the Specific Plan land use designations. The applicant also proposes to construct 2 industrial buildings with mezzanines on separate parcels totaling 1,820,000 square feet on (combined) 115.88 acres, located northerly of (future roads) Bunker Hill Drive, easterly of Airman Drive, southerly of Arclight Drive, and westerly of Linebacker Drive. (Only development entitlements for PP21-03 and PP21-04 have been submitted with this application. No development projects for the other parcels have been proposed at this time.) The applicant also proposes a tentative tract map to divide 359.6 acres into 17 buildable lots and 7 lettered lots for streets/open space. (Airport Compatibility Zones B1, B2, C1, and C2 High Terrain Zone of the March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

4.0 PUBLIC HEARING: MISCELLANEOUS ITEMS

None

5.0 ADMINISTRATIVE ITEMS

5.1 Director's Approvals – Information Only

5.2 Update March Air Reserve Base Compatibility Use Study (CUS)

Simon Housman, Project Director March CUS expressed that they recently completed the first gathering of the senior staff of the various stakeholders to go over the initial findings that the consultant Matrix Design Group has developed.

5.3 Digitizing the ALUC Application Process

Jackie Vega, ALUC staff presented Power Point slides to the Commission indicating plans to move ALUC forward towards digitization, including updating and digitizing the ALUC application process.

6.0 APPROVAL OF MINUTES

Chair Manos motioned to approve the April 14, 2022 minutes. Seconded by Commissioner Steven Stewart. Abstained: Commissioners Froehlich and Geller. (Vote 4-0)

7.0 ORAL COMMUNICATION ON ANY MATTER NOT ON THE AGENDA

None

8.0 COMMISSIONER'S COMMENTS

Commissioner Geller who attended ALUC meetings with his personal guide dog expressed that this will be his dog last meeting attendance.

9.0 ADJOURNMENT

Steve Manos, Chair adjourned the meeting at 11:02 a.m.

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VIDEO:

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PAGE BREAK





**AIRPORT LAND USE COMMISSION MEETING
MINUTES
June 9, 2022**

DRAFT

6-13-22

COMMISSIONERS PRESENT: Michael Geller, John Lyon, Steve Manos, Steven Stewart, Richard Stewart

COMMISSIONERS ABSENT: Russell Betts

2.0 PUBLIC HEARING: CONTINUED ITEMS

None

3.0 PUBLIC HEARING: NEW CASES

- 3.1 Staff report recommended: **CONDITIONALLY CONSISTENT**
Staff recommended at hearing: **CONDITIONALLY CONSISTENT**
ALUC Commission Action: **CONTINUE to 7-14-22 (Vote 4-1, Manos dissenting)**
Motion: John Lyon
Second: Steven Stewart
- ZAP1026PV22 – Richland Developers, Inc (Representative: Derek Barbour)** – City of Perris Case No. DPR 22-00002 (Development Plan Review), TPM38446 (Tentative Parcel Map). A proposal to construct two industrial buildings with mezzanines on two separate parcels totaling 659,130 square feet on 37.55 acres, located on the northeast corner of Goetz Road and Mapes Road. The applicant proposes to divide the site into two parcels (Airport Compatibility Zones B2 and D of the Perris Valley Airport Influence Area, and Zone E of March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org. Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org
- 3.2 Staff report recommended: **CONSISTENT**
Staff recommended at hearing: **CONSISTENT**
ALUC Commission Action: **CONSISTENT (Vote 5-0)**
Motion: Richard Stewart
Second: Michael Geller
- ZAP1088BD22 – City of Indio (Representative: Kevin Snyder)** – City of Indio Case No. ZTA2022-2 (General Plan Amendment, Zoning Code Amendment). A City-initiated proposal to amend its General Plan 2040 in response to State housing legislation, including changes to the Land Use Element: creating 2 new land use designations (Suburban Neighborhood Low and High, and Connect Neighborhood Low and High); incorporating the new Desert Estates Transition land use designation, redesignating parcels to the new land use designations, and clean up parcels to address zoning consistency, property ownership, and existing uses on the ground. The City also proposes to update its Unified Development Code/Zoning Code to be consistent with the General Plan (All Compatibility Zones [A, B1, B2, C, D, and E] of the Bermuda Dunes Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org. Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

VIDEO:

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**AIRPORT LAND USE COMMISSION MEETING
MINUTES
June 9, 2022**

4.0 PUBLIC HEARING: MISCELLANEOUS ITEMS

None

5.0 ADMINISTRATIVE ITEMS

5.1 Director's Approvals - Information Only

5.2 Update March Air Reserve Base Compatibility Use Study (CUS)

In the absence of Simon Housman, Project Director March CUS, Paul Rull, ALUC Director informed the Commission that our first public workshop for the March CUS Project will be held on Wednesday, June 15 at the March Field Air Museum. The ALUC Commissioners were also invited to attend the meeting.

6.0 APPROVAL OF MINUTES

Due to a lack of quorum, Chair Manos motioned to continue the May 12, 2022 minutes to the next ALUC meeting on July 14, 2022.

7.0 ORAL COMMUNICATION ON ANY MATTER NOT ON THE AGENDA

Paul Rull, ALUC Director announced and congratulated Commissioner Michael Geller who was successfully reappointed by the County Board of Supervisors on May 24, 2022 for another 4 year term with ALUC.

8.0 COMMISSIONER'S COMMENTS

None

9.0 ADJOURNMENT

Steve Manos, Chair adjourned the meeting at 10:31 a.m.

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VIDEO:

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