

**COUNTY OF RIVERSIDE  
AIRPORT LAND USE COMMISSION**

**STAFF REPORT**

**AGENDA ITEM:** 2.1 2.2 4.6

**HEARING DATE:** August 13, 2020 (continued from July 9, 2020, June 11, 2020, May 14, 2020)

**CASE NUMBER:** ZAP1405MA20 – Riverside Inland Development, LLC./Hillwood Investment Properties (Representative: Kathy Hoffer)

**APPROVING JURISDICTION:** March Joint Powers Authority (MJPA)

**JURISDICTION CASE NOS:** SP16-02 (Specific Plan), PP20-02 (Plot Plan), PM20-02 (Tentative Parcel Map No. 37220), (“VIP 215”)

**LAND USE PLAN:** 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

**Airport Influence Area:** March Air Reserve Base

**Land Use Policy:** Zone B2

**Noise Levels:** 65 – 75 range CNEL from aircraft

**MAJOR ISSUES:** Security, drainage, potential for glare and storage of hazardous materials in close proximity to the military runway were previous concerns identified by the Air Force in the original project, and had requested that these concerns be addressed in the project EIR, ~~which has not yet been released.~~ The proposal has been redesigned from a two-building to a single building project. ~~At the time this staff report was written, the Air Force has not completed its review of the new proposed project. The Air Force provided comments that were submitted to the Commission at the May 14, 2020 meeting. These comments included the following:~~

- *Concur with April 9, 2020 ALUC staff report findings (received via e-mail on 3/20/20) citing previous MARB concerns (via AF letters in Sept. 2016 and 2018) raised for security, drainage, glare, safety/HAZMAT storage, and Aviation land use/zoning.*
- *Concur with latest redlined Specific Plan report (received via e-mail on 4/23/20) that developer shall comply with previously identified BASH concerns.*
- *However: 1) MARB has responded with comment to March JPA concerning Draft EIR that NEPA will be required for Developer’s proposed interim drainage channel solution, and 2) MARB cannot approve any proposed long term drainage channel solution on behalf of Riverside County Flood Control until NEPA is complete for the proposed interim*

***solution.***

***It was the nature and extent of the last Air Force bullet comment that prompted ALUC staff to recommend a continuance from the May 14 meeting to June 11. ALUC staff wanted time to clarify Air Force's concerns with the proposed project's interim drainage channel solution and the underlying National Environmental Policy Act requirement.***

***In addition, the Air Force submitted additional comments in the form of a letter dated May 11, 2020, which contained response to comments to the project's Environmental Impact Report processed by the MJPA. This letter re-emphasizes the Air Force's concern with the project's stormwater runoff affecting the Base. The letter also includes comments regarding burrowing owl (species of special concern) populations and habitat on the project site, and raising concerns that development of the site could push these populations onto the Base, creating a potential hazard to flight.***

***Based on the fact that the Air Force has significant concerns with the proposed drainage plan and the extent of regional stormwater flooding issues at the Base, these unresolved issues could potentially result in wildlife attractant and a hazard to flight. Therefore, until the Air Force has been satisfied that their concerns have been addressed, ALUC staff is recommending continuance off-calendar. The applicant had requested at the July 9 ~~June 11~~, 2020 meeting that the item be continued to the date specific meeting of August 13 ~~July 9~~, 2020, in order to address the concerns of the Air Force.***

**The project includes 6.5 acres of bio-retention and bio-swale areas. Bioretention areas are not recommended in the vicinity of airports due to the potential that such areas could provide food, water, and shelter for hazardous wildlife causing a Bird Aircraft Strike Hazard (BASH). A study of the site was performed by an FAA-qualified airport wildlife biologist and a wildlife hazard review study was prepared. The study identifies that March Air Reserve Base has historically experienced wildlife strikes, with ninety-two (92) strikes occurring between 2007 and 2019. The study analyzed the proposed bioretention basin and landscaping design, and recommends that the basin be constructed with 4:1 slopes (which will help prevent entry and nesting by potentially hazardous wildlife), and that the basin's sides and bottom will use hardscapes like rock scape (in lieu of plantings), which will remove food sources, cover, and nesting cover, making the basin less attractive to wildlife. The study also recommends the incorporation of landscape design policies that *are is* consistent with the ALUC wildlife/landscaping brochures in the underlying specific plan. These policies are included in the updated Specific Plan. The study concludes that the project would be able to achieve consistency with the airport land use compatibility plan regarding wildlife attractants and hazards to flight.**

**RECOMMENDATION: Staff recommends that the Commission CONTINUE the matter to the September 10 ~~August 13 June 11~~ 2020, meeting, pending completion of the Air Force review of the project ~~off-calendar, until such time as the~~ and that the Air Force is satisfied that their concerns have been addressed. ALTERNATIVE RECOMMENDATION: If the applicant will not**

*consent to the continuance, staff must recommend a finding of **INCONSISTENT**, based on the cumulative impact of this large scale development in its entirety.*

**PROJECT DESCRIPTION:** The applicant proposes to construct a 2,022,364 square foot industrial warehouse building (with a maximum building height of 54 feet) with mezzanines on 142.5 acres. The applicant also proposes to change the Veterans Industrial Park 215 Specific Plan (SP16-02), updating Section 4.3 Landscaping Guidelines to reflect ALUC wildlife hazard goals and policies. The applicant also proposes to merge the project's five parcels into one parcel.

The Commission had previously determined the original two building project consistent (by a 4-3 vote) through its action on ZAP1274MA17 at its October 11, 2018 hearing. A new ALUC application was required because of the proposal to increase the building height, the inclusion of second floor mezzanine area, and the overall redesign of the site from two buildings to a single building project.

**PROJECT LOCATION:** The site is located easterly of Interstate 215, southerly of March Air Force Base Museum and easterly terminus of Van Buren Boulevard, northerly of Nandina Avenue, and westerly of March Air Reserve Base, within the jurisdiction of the March Joint Powers Authority, approximately 950 feet westerly of Runway 14-32 at March Air Reserve Base.

#### **BACKGROUND:**

##### Original Determined Consistent Project ZAP1274MA17:

The Commission found the original project on this site consistent by a 4-3 vote on October 11, 2018. The original project proposed two industrial buildings (with a maximum building height of 48 feet) totaling 2,185,618 square feet on 142.5 acres, and also proposed to amend the March Joint Powers Authority General Plan to include general warehousing/logistics uses as allowable land uses on lands designated as "Aviation" (AV), to amend the site's designation from "AV" to "AV (SP-8)", and to update the Building Capacity table in the Land Use Element. Specific Plan No. 16-02 proposed a new Specific Plan (SP-8) providing goals, policies, programs, land uses, development standards, and design guidelines for development on this site. Tentative Parcel Map No. 37220 proposed dividing the site into two parcels (one for each building).

A copy of the original staff report (ZAP1274MA17) has been included in this package to provide an overview of the previous issues, concerns, analysis and comments brought up during the project.

#### **CURRENT PROPOSED PROJECT:**

The current proposed project increases the building height to 54 feet, adds a second floor mezzanine and has a single building.

Non-Residential Average Land Use Intensity: Pursuant to the Airport Land Use Compatibility Plan

for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zone B2, which limits average intensity to 100 people per acre.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, the following rates were used to calculate the occupancy for the proposed building in Compatibility Zone B2:

- Office – 1 person per 200 square feet (with 50% reduction)
- Warehouse – 1 person per 500 square feet

The project proposes a 2,022,364 square foot industrial warehouse building, which includes 1,962,221 square feet of warehouse area, 46,637 square feet of first floor office area, and 13,506 square feet of second floor office mezzanine area, accommodating an occupancy of 4,225 people, which would result in an average intensity of 30 people per acre, which is consistent with the Compatibility Zone B2 criterion of 100.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle and 1.0 persons per truck trailer parking in the absence of more precise data). Based on the 634 parking stalls and 428 truck trailer stalls provided, the total occupancy would be estimated to be 1,379 people. The resulting average intensity of 10 people per acre is consistent with the Compatibility Zone B2 average criterion of 100.

Non-Residential Single-Acre Land Use Intensity: Compatibility Zone B2 limits maximum single-acre intensity to 250 people. There are no risk-reduction design bonuses available, as March Air Reserve Base/Inland Port Airport is primarily utilized by large aircraft weighing more than 12,500 pounds.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre area would include 22,568 square feet of warehouse area, 20,992 square feet of first floor office area, and 13,506 square feet of second floor office mezzanine area, accommodating 218 people, which is consistent with the Compatibility Zone B2 single acre criterion of 250. Due to the addition of the mezzanine area, a condition must be added prohibiting conversion of warehouse areas to manufacturing use within 210 feet of any mezzanine area.

March Air Reserve Base/United States Air Force Input: Given that the project site is located in Zone B2 and immediately adjacent to the primary runway at March Air Reserve Base (MARB), the March Air Reserve Base staff was notified of the revised project and sent a package of plans for their review. ~~As of the time this staff report was prepared, we were still awaiting comments from the Air Force regarding this revised project.~~

The MARB staff previously reviewed the original project and provided comment letters dated September 2016 and September 2018, which identified several concerns including security, drainage,

glare, and safety. These issues, and comments from ALUC staff and the applicant were discussed in the following table. *(This table has been updated to include the latest comments from the Air Force).*

<b>Issues</b>	<b>Air Force</b>	<b>ALUC</b>	<b>Applicant</b>
Security	Cameras shall not face or record any actions or portion of the base operations. Perimeter fencing shall be constructed out of block.	<p>In addition to the Air Force comment regarding security, ALUC staff raised the concern of the project's size and proximity to the runway with regards to the Base's Anti-Terrorism Force Protection and their ability to carry out its mission of protecting the base. Specifically, since the project expands the use of the site to include non-aviation use, ALUC staff is concerned with the ability of Base security personnel to immediately respond to a threat at the project facility. Therefore it is recommended that the master lease and sublease include provisions that enable Base security personnel to respond immediately to what they perceive as a possible risk at the project facility.</p> <p><b><u>Approved ALUC condition #11 states that the lease between the MJPA and the applicant or future tenants shall include that the Air Force has the right and authority to inspect the premises without prior notice as needed for security of its operations.</u></b></p>	No cameras will be oriented towards the runway and cameras will not record base operations. Fencing along the project-airport boundary shall be minimum 8 feet in height with three strands of barbed wire, and shall be of a durable material subject to the MJPA and Base review. This type of fencing was requested by the Base Security Forces. The project will not impede Base's mission to carry out Anti-Terrorism Force Protection procedures. The applicant will work with their legal counsel to craft the appropriate commercially reasonable language for lease agreements regarding Base security forces to access the property during an imminent threat (same access as provided to law enforcement and emergency response teams) while providing reasonable notice to tenants absent an imminent threat.
Drainage	Rising groundwater table at the	In addition to the Air Force comment regarding drainage, ALUC staff supports the Base's	The Specific Plan identifies how the project will address storm water drainage to be consistent with NPDES

	<p>base is an ongoing concern, specifically, the ability of a project to drain water detention basins within 48 hours. Base staff shall review basin design. These basins shall address Bird Wildlife Aircraft Strike Hazard concerns.</p> <p><i>Air Force reiterates its previous concerns regarding rising groundwater table and the need for an interim drainage channel solution.</i></p>	<p>request to review all drainage plans prior to approval.</p> <p><i>No resolution on the drainage channel solution could result in a bird wildlife hazard.</i></p>	<p>and WQMP requirements. The applicant has met with MIPA and Base staff in discussing proposed drainage solutions. Applicant's drainage studies indicates the downstream floodplain limits are less (than historical flow and current condition) due to the project's improvements.</p> <p>No drainage solution has been agreed upon yet (however, this will most likely occur during the Base's review of the project's EIR).</p> <p><i>The applicant is still in the process of satisfying the Air Force comment regarding drainage, possibly in the form of an agreement that identifies funding and implementation of the interim drainage solution.</i></p>
Glare	<p>Solar panels or any reflective materials on the rooftop are prohibited. Construction material shall</p>	<p>In addition to the Air Force comment regarding glare, ALUC staff notes that the project does not propose solar panels at this time. Any future solar panels could potentially result in significant glare impacts, and therefore, a solar glare hazard analysis would be</p>	<p>The Specific Plan indicates that materials shall be of a non-reflective material, and that highly reflective materials on elevations facing the runway or aircraft approach path are prohibited. Solar panels are prohibited.</p>

	<p>be non-reflective including outside ductwork, windows, and roofs.</p>	<p>required to analyze the impacts.</p> <p>In the event of any reasonable complaint about glare related to aircraft operations, the applicant shall agree to such specific mitigation measures as determined or requested by MARB.</p> <p><b><u>Approved ALUC condition #10 states no solar panels are proposed as part of the project. If solar panels are proposed, a solar glare hazard study and additional ALUC review would be required.</u></b></p>	
Safety	<p>No hazardous materials shall be stored within the facility.</p>	<p>No additional comments to the Air Force Comment regarding safety.</p> <p><b><u>Approved ALUC condition #8 prohibits the storage, process, or manufacture of hazardous materials without ALUC approval.</u></b></p>	<p>The Specific Plan prohibits above ground petroleum storage containers and below ground storage containers in excess of 10,000 gallons.</p>
Aviation Land Use	<p>Not identified.</p>	<p>The project expands the permissible use of the property from aviation only to include non-aviation uses. The applicant shall agree to conditions being placed on the project that will preserve the ability for subsequent aviation use and to construct taxiways and access to the runways.</p> <p>Recommended condition is included that states “the project shall not pre-empt future opportunities for the extension of taxiway access to the runway</p>	<p>The Specific Plan identifies the site as un-zoned. The establishment of the Specific Plan will provide a designation on the MJPA zoning map with an underlying Aviation Designation. Under the Specific Plan, light manufacturing and assembly uses including aviation related manufacturing is a permitted use. One of the key project objectives identified in the Specific Plan is to “facilitate the development of underutilized land currently planned for aviation-related uses that maximize the use of the site and responds to market demand within the Specific Plan area and surrounding region for a large</p>

		from the site”.	format logistics center.”
		<b><u>Approved ALUC condition #12 states that the project shall not pre-empt future opportunities for the extension of taxiway access to the runway from the site.</u></b>	The project will not obstruct future aviation use or the right to obtain taxiway access to the runway from the project.
<b>Natural Resource</b>	<i>New comments have been provided regarding burrowing owl populations and habitats being displaced from the project site and onto the Base. Additional resources should be analyzed are Stephens Kangaroo Rat and vernal pools fairy shrimp.</i>	<i>Although ALUC does not review natural resources as a source of environmental protection, it does have concerns if these biological resources result in an increase in bird/wildlife hazards.</i>	

It should be noted that the above issues, among others, will be considered in the upcoming ~~Joint~~ **Land Compatible Use Study (JLUS CUS)** requested by the Air Force and supported by the March JPA and other local jurisdictions.

Due to the project’s size and close proximity to the runway, it could potentially impact the JLUS CUS review process or ability to implement its conclusions. As an example, the JLUS CUS will look at increased glare from passive reflective roof surfaces, such as the two million square feet of roof surface on this proposed project.

**The Air Force provided comments that were submitted to the Commission at the May 14, 2020, meeting. These comments included the following:**



- **Concur with April 9, 2020 ALUC staff report findings (received via e-mail on 3/20/20) citing previous MARB concerns (via AF letters in Sept. 2016 and 2018) raised for security, drainage, glare, safety/HAZMAT storage, and Aviation land use/zoning.**
- **Concur with latest redlined Specific Plan report (received via e-mail on 4/23/20) that developer shall comply with previously identified BASH concerns.**
- **However: 1) MARB has responded with comment to March JPA concerning Draft EIR that NEPA will be required for Developer's proposed interim drainage channel solution, and 2) MARB cannot approve any proposed long term drainage channel solution on behalf of Riverside County Flood Control until NEPA is complete for the proposed interim solution.**

**It was the nature and extent of the last Air Force bullet comment that prompted ALUC staff to recommend a continuance from the May 14 meeting to June 11. ALUC staff wanted time to clarify Air Force's concerns with the proposed project's interim drainage channel solution and the underlying National Environmental Policy Act requirement.**

**The Air Force also provided to ALUC their comment letter dated May 11, 2020, in response to the project's underlying Environmental Impact Report (EIR) that is being processed by the March Joint Powers Authority (MJPA). The comment letter expresses concerns with the project's short-term and long-term solutions to the stormwater flooding issues impacting the Base, thus needing a National Environmental Policy Act environmental assessment. The Air Force indicates that a regional storm water project is the main solution to the flooding problems (caused by adjacent developments) affecting the Base. As of this time, no plans for funding or implementation of the regional storm water project has been presented to the Base. The letter also includes comments regarding burrowing owl (an endangered species of special/concern) populations and habitat on the project site, and raising concerns that development of the site could push these populations onto the Base, creating a potential hazard to flight.**

**Based on the fact that the Air Force has significant concerns with the proposed drainage plan and the extent of regional stormwater flooding issues at the Base, these unresolved issues could potentially result in wildlife attractant and a hazard to flight. Therefore, until the Air Force has been satisfied that their concerns have been addressed, ALUC staff recommends continuance off-calendar. *The applicant had requested at the July 9 June 11, 2020 meeting that the item be continued to the date specific meeting of August 13 July 9, 2020, in order to address the concerns of the Air Force.***

Prohibited and Discouraged Uses: Compatibility Zone B2 prohibits children's schools, day care centers, libraries, hospitals, congregate care facilities, hotels/motels, places of assembly, highly noise-sensitive outdoor nonresidential uses and hazards to flight. The applicant does not propose any within the project; however, staff is concerned as to the potential for the proposed bio-retention basins to become bird attractants. (See discussion, below.)

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being in an area within the 65-75 CNEL range from aircraft noise. As a primarily industrial use not sensitive to noise (and considering typical anticipated building construction noise attenuation of approximately 20 dBA), the warehouse area would not require special measures to mitigate aircraft-generated noise. However, a condition is included to provide for adequate noise attenuation within office areas of the building.

Part 77: The elevation of Runway 14-32 at its southerly terminus is 1,488 feet above mean sea level (1,488 feet AMSL). At a distance of approximately 950 feet from the runway to the closest parcel within the site, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,497.5 feet AMSL.

The original proposal for a 48 foot tall building was reviewed by the FAA Obstruction Evaluation Service (FAA OES), and Determination of No Hazard to Air Navigation letters (Aeronautical Study Numbers 2016-AWP-12028 thru 2016-AWP-12036-OE) were issued on January 26, 2017, revealing that the project's structures would not exceed obstruction standards and would not be a hazard to air navigation provided conditions are met. Due to the close proximity of the buildings to the runway, marking and lighting of the northeast and southeast corners of each building were required by the FAA OES.

The project proposes increasing the maximum building height to 54 feet and the maximum top point elevation to 1,578 feet AMSL, triggering a new review of the building by the FAA OES. A new submittal to the FAAOES was made and Aeronautical Study Numbers 2020-AWP-644-OE to 2020-AWP-649-OE were assigned. Determination of No Hazard to Air Navigation letters were issued on February 27, 2020. The FAA OES determined that the project would not result in an impact to air navigation, provided that the project complies with the conditions in that letter (which have been included in staff's recommended conditions). The FAA OES also determined that marking and lighting, which were required in the original project, would not be necessary for the proposed project.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (**Section 2.3.2 of FAA Advisory Circular 5200-33B C**)

The project includes 6.5 acres of bio-retention and bio-swale areas. Bioretention areas are not recommended in the vicinity of airports due to the potential that such areas could provide food, water, and shelter for hazardous wildlife. In order to evaluate this potential, the applicant team has commissioned a wildlife hazard study from a qualified wildlife hazard biologist.

On March 16, 2020, Mead and Hunt submitted a wildlife hazard review study (“the Study”) of the proposed project’s stormwater and landscape plans, and a study of the site was performed by an FAA-qualified airport wildlife biologist. The Study provides research data on wildlife strikes at March Air Reserve Base, with ninety-two (92) strikes occurring during a 13 year period between 2007 and 2019, with songbirds, swallows, swifts, and raptors being the most commonly struck birds identified. Biological surveys of the project site were conducted in 2015, 2018, and 2019, where doves, sparrows, songbirds and raptors were identified (all of which were identified in the FAA list of the 25 most hazardous species to aircraft operations).

The Study analyzed the proposed bioretention basin located adjacent to the eastern project boundary and parallel to the runway. The basin will be constructed with 4:1 slopes (which will help prevent entry and nesting by potentially hazardous wildlife) and is sized to collect and convey 100-year storm event, discharging within 48 hours after the end of a storm event. The basin’s sides and bottom will use hardscapes like rock scape (in lieu of plantings) which will remove food sources, cover, and nesting cover, making the basin less attractive to wildlife.

The Study also analyzed the proposed landscaping design as plant selections, density, and planting configuration can influence wildlife use, abundance, and behavior, especially landscaping near stormwater management facilities. As such, the project has been conditioned for the proposed landscaping to be consistent with the ALUC brochures titled “Landscaping near Airports” and “Airports, Wildlife and Stormwater Management” which should reduce the potential for wildlife attractants.

The Study recommends that the project’s underlying VIP 215 Specific Plan be updated to follow the guidelines of the ALUC landscaping brochures, which are now included in the Specific Plan.

In addition, Mead & Hunt recommends that Section 4 of the VIP 215 Specific Plan be revised as follows to promote consistency with the 2014 ALUCP and ALUCP design guidance:

- Section 4.3.1 should be revised to reflect the goals of the ALUC for landscaping within the AIA and set forth in its guidance “Landscaping Near Airports.” The section should include a revised version of Table 4-1 that reflects the memo from Hunter Landscaping dated March 5, 2020, and the recommendations cited above for trees, shrubs, and groundcover.
- Section 4.3.1 should be revised to state that subsequent landscape plans created by tenants for portions of the VIP site must adhere to the Specific Plan and plant materials identified and guidance set forth by the ALUC and the Applicant’s goal of using only plant materials that are acceptable following review by a QAWB. This language should be included in development agreements as well.
- Section 4.3.2 should be revised to reflect the use of hardscape for proposed stormwater management basins.

The Study concludes that with the incorporation of the above recommendations, the proposed project would be able to achieve consistency with the airport land use compatibility plan regarding wildlife attractants and hazards to flight.

**CONDITIONS:**

1. Any outdoor lighting installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, congregate care facilities, hotels/motels, places of assembly (including churches and theaters), buildings with more than 3 aboveground habitable floors, noise sensitive outdoor nonresidential uses, critical community infrastructure facilities and hazards to flight.
3. Prior to issuance of any building permits, the landowner shall convey and have recorded an avigation easement to the March Inland Port Airport Authority. Contact March Joint Powers Authority at (951) 656-7000 for additional information.

4. The attached notice shall be provided to all prospective purchasers of the property and tenants or lessees of the building.
5. Any ground-level or aboveground water detention basin or facilities, including water quality management basins, shall be designed and maintained for a maximum 48-hour detention period after the design storm and remain totally dry between rainfalls. Vegetation around such facilities that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced to prevent contiguous canopy, when mature. Trees and bushes shall not produce fruit, seeds, or berries.

Landscaping in the detention basin, if not rip-rap, shall be in accordance with the guidance provided in ALUC's "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at [WWW.RCALUC.ORG](http://WWW.RCALUC.ORG) which list acceptable plants from Riverside County Landscaping Guide, or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

6. March Air Reserve Base (MARB) personnel must be transmitted for their review and approval details of the storm water conveyance system and landscaping plans.
7. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
8. This project has been evaluated for 1,962,221 square feet of warehouse area, 46,637 square feet of first floor office area, and 13,506 square feet of second floor office mezzanine area. Any proposals for manufacturing uses, showrooms, retail trade, and/or employee support uses such as cafeterias, training facilities, exercise rooms, or conference rooms, or any changes to the interior floor layout plan shall require subsequent review by the Airport Land Use Commission. In addition, this project shall not store, process or manufacture hazardous materials without review and approval by the Airport Land Use Commission.
9. Noise attenuation measures shall be incorporated into the design of the office areas of the proposed building, to the extent such measures are necessary to ensure that interior noise levels from aircraft operations are at or below 45 CNEL.
10. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission and March Air Reserve Base. In the event of any reasonable complaint

about glare related to aircraft operations, the applicant shall agree to such specific mitigation measures as determined or requested by MARB.

11. The lease between the March Joint Powers Authority and the applicant (or any future tenants) shall include that the U.S. Air Force has the right and authority to inspect the premises without prior notice as needed for security of its operations and personnel in its sole discretion.
12. The project shall not pre-empt future opportunities for the extension of taxiway access to the runway from the site.
13. Any roof-top equipment or change in height that exceeds a total height of 54 feet will require Form 7460-1 submittal, review, and issuance of a "Determination of No Hazard to Air Navigation" by the Federal Aviation Administration Obstruction Evaluation Service.
14. The Federal Aviation Administration has conducted aeronautical studies of the proposed project (Aeronautical Study Nos. 2020-AWP-644 through 2020-AWP-649-OE) and has determined that neither marking nor lighting of the structure is necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 L Change 2 and shall be maintained in accordance therewith for the life of the project.
15. The proposed building shall not exceed a height of 54 feet above ground level and a maximum elevation at top point of 1,578 feet above mean sea level.
16. The maximum height and top point elevation specified above shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in structure height or elevation shall not require further review by the Airport Land Use Commission.
17. Temporary construction equipment used during actual construction of the structure(s) shall not exceed 54 feet in height and a maximum elevation of 1,578 feet above mean sea level, unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.
18. Within five (5) days after construction of the proposed building reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned or a decision is made not to construct the applicable structure.

**COUNTY OF RIVERSIDE  
AIRPORT LAND USE COMMISSION**

**DIRECTORS SUPPLEMENT TO THE STAFF REPORT.**

AGENDA ITEM: 2.2  
HEARING DATE: July 9, 2020 (Continued from June 11, 2020, May 14, 2020)  
CASE NUMBER: ZAP1405MA20 - Riverside Inland Development, LLC/Hillwood Investment Properties (Representative: Kathy Hoffer)  
APPROVING JURISDICTION: March Joint Powers Authority (MJPA)  
LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan. (MARCH ALUCP)  
Airport Influence Area: March Air Reserve Base  
Land Use Policy: Zone B2  
Noise Levels: 65-75 range CNEL from aircraft.  
PROPERTY LOCATION: 950 FEET WEST OF RUNWAY 14-32, Main Runway, 13,302 long, 200 feet wide.

To the Chairman and Members of the Riverside County Airport Land Use commission: The Staff Report submitted to the RCALUC, of even date, is incorporated in full by this reference. The Staff Report includes the customary detailed analysis and comments by the March ARB raising issues impacting the viability of operations on a military installation. The purpose of this Directors Supplement to the Staff Report is to provide background and context.

**1. SUMMARY CONCLUSION, EXECUTIVE SUMMARY AND STAFF RECOMMENDATION.**

Staff recommends that the Commission CONTINUE the matter to the August 13, 2020 meeting pending completion of the Air Force review of the project *and that the Air Force is satisfied that their concerns have been addressed. If the applicant will not consent to the continuance the Staff must recommend a finding of INCONSISTENT based on the cumulative impacts of this large scale development in its entirety.*

- A. Two Million Square Foot warehouse plus additional hard surfaced parking impacts.
- B. Drainage retention basins attracting Bird Aircraft Strike Hazard (BASH).
- C. Visual roof glare only 950 feet from the runway impairing pilot vision.
- D. Light emissions interfering with pilot Night Vision Goggle training.
- E. Security risk - loaded tractor trailers 950 feet from the runway but outside the AOA.
- F. Circumventing the Compatible Use Study (formerly JLUS).
- G. Not protecting the long term viability of March ARB from future BRAC.

1. **THE POWERS AND DUTIES OF THE RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION ARE BROADER THAN JUST THE MATHEMATICAL CALCULATION OF INTENSITY OF USE.**

“The commission has the following powers and duties, subject to the limitations upon its jurisdiction set forth in Section 27676:(a)...(b) To coordinate planning at the state regional, and local levels so as to provide for the orderly development of air transportation, while at the same time protecting the public health, safety and welfare...” California Public Utilities Code Sec. 21674.

A. **THE INTENSITY NUMBERS ARE CONSISTENT WITH THE MATHEMATICAL TOOL. BUT USING THE PROPERTY FOR THE LARGEST POSSIBLE LOW INTENSITY BUILDING IS ITSELF INCONSISTENT WITH THE GOALS AND STATUTORY AUTHORITY OF THE RCALUC.**

The mathematical calculation of the building intensity based on the stated use and square footage of the building is only one tool used by the staff to evaluate the overall compatibility of projects at all airports in Riverside County. A warehouse is a low intensity use and using that standard alone the proposed two million square foot warehouse/e-commerce building is consistent with permissible intensity in Zone B-2 of the March ALUCP. If the analysis required only a mathematical calculation all matters could be determined mathematically by staff without need for a Commission.

**The role of the RCALUC is broader: “to provide for the orderly development of air transportation, while at the same time protecting the public health, safety and welfare...”** California Public Utilities Code Sec. 21674.

It is not a mistake that the PUC has aviation as the focus of this sentence! The MARCH ALUCP imposes compatibility criteria beyond the intensity mathematical calculation to prohibit uses that increase the risk to aviation operations.

B. **REVIEWING THE TWO MILLION SQUARE FOOT BUILDING SURROUNDED BY A HARD SURFACE PARKING AND TRUCK LOADING ZONE LOCATED 950 FEET FROM THE MAIN RUNWAY AT THE JOINT USE MILITARY-CIVILIAN AIRPORT RAISES MANY SITE SPECIFIC ISSUES. THE DEVELOPMENT MUST SATISFY THE MARCH ALUCP AND THE MILITARY REQUIREMENTS IN THE AICUZ.**

The SPECIFIC PLAN AMENDMENT (SP-8) raises many issues and impacts on the “Orderly development of Air Transportation” and “Protecting the health safety and welfare of the public” which may or may not be adequately resolved or mitigated. ALL OF THOSE ISSUES AND IMPACTS ARE WITHIN THE STATUTORY POWERS AND DUTIES OF THE ALUC.



The MARCH ALUCP provides for these impacts.

*“2.5 Supporting Compatibility Criteria fo Airspace Protection:*

....

(d) Countywide Policy 4.3.7: Additional hazards to flight as listed in Table MA-2 are to be avoided in the vicinity of March ARB/IPA” (MARCH ALUCP, Page 5)

Table MA-2 of the MARCH ALUCP, lists the compatibility policies for Zone B2, including the VIP 215 property. Table MA-2 has a column of Prohibited Uses and the column title refers to Table note 3. “The uses listed here are ones that are explicitly prohibited regardless of whether they meet the intensity criteria.” (Emphasis added.)

Among the Prohibited Uses are “Hazards to Flight” referring to Table note 8.

“**Hazards to flight include physical (e.g. tall objects), visual, and electronic forms of interference with the safety of aircraft operations. Land use development that may cause the attraction of birds to increase is also prohibited.** Man-made features must be designed to avoid heightened attraction of birds. In Zones A, B1 and B2, flood control facilities should be designed to hold water for no more that 48 hours following a storm and be completely dry between storms. (see FAA Advisory Circular 150/5200-33B). ... See also Countywide Policy 4.3.7.” (Emphasis Added.)

Countywide Policy 4.3.7 states:

“**Other Flight Hazards: New land uses that may cause visual, electronic, or increased bird strike hazards to aircraft in flight shall not be permitted within any airport’s influence area.** Specific characteristics to be avoided include: (a) Glare or distracting lights which could be mistaken for airport lights; (b) Sources of dust, steam, or smoke which may impair pilot visibility; (c) Sources of electrical interference with aircraft communications or navigation; and (d) Any proposed use, especially landfills and certain agricultural uses, that creates an increased attraction for large flocks of birds. (Refer to FAA Order 5200.5A, *Waste Disposal Sites on or Near Airports* and Advisory Circular 150/5200-33A, *Hazardous Wildlife Attractants On or Near Airports.*) (Superseded by Advisory Circular 150/5200-33C, 2/21/2020, Sec. 2.3.2.) (Emphasis added.)

- C. NEW DEVELOPMENT THAT ATTRACTS BIRDS IS PROHIBITED BY THE MARCH ALUCP AND CONTRARY TO THE RECOMMENDATION OF THE AIR FORCE IN THE AICUZ.

Bird Aircraft Strike Hazard (BASH) study was requested and two version provided.

The MARCH ALUCP classifies anything, such as a retention basin, that may cause the attraction of birds as “Prohibited.” The BASH arises from approximately 6 acres of retention basins located along the March ARB fence line, at the point closest to the runway.

The SPA explains the Retention Basins at page 2-15 (pg. 33 of the SPA pdf file).

“The soil is impermeable and infiltration rates have been determined to be less than 1.6 inches/hour. Harvest and Use is not utilized because the anticipated demands for irrigation and toilet use are less than their respective required amounts. Therefore, bio-retention BMPs are considered for this site.”

“Thus the primary BMP’s to be implemented will be to construct three bio-retention/bio-treatment basins.”

The SPA explains that “Hydro-modification refers to changes in the runoff characteristics caused by altered land use and increase in impervious areas.” SPA Section 2.3.4 Grading, states: “The grading plan for the site creates building pads, parking area and bio-retention basins.” Page 2-17,(Page 34 of the SPA pdf file.)

The size of the retention basins is controlled by the size of the impervious areas required by the specific design of the project, a two million square foot building and surrounding parking areas.

**2. THE AIRPORT LAND USE COMPATIBILITY PLAN FOR MARCH AIR RESERVE BASE (MARCH ALUCP) IS REQUIRED TO BE CONSISTENT WITH THE SAFETY AND NOISE STANDARDS OF THE AIR INSTALLATION COMPATIBLE USE ZONE (AICUZ).**

“The commission shall include, within its airport land use compatibility plan formulated pursuant to subdivision (a), the area within the jurisdiction of the commission surrounding any military airport for all of the purposes specified in subdivision (a). The airport land use compatibility plan shall be consistent with the safety and noise standards in the Air Installation Compatible Use Zone (AICUZ) prepared for the military airport. This subdivision does not give the commission any jurisdiction or authority over the territory or operations of any military airport. California Public Utilities Code Sec. 21675(b).”

**A. THE BASH CHARACTERISTICS OF THE PROPOSED PROJECT IS INCONSISTENT WITH THE 2018 AICUZ.**

The 2018 MARCH ARB AICUZ states in part:

“To reduce the potential of a BASH, the Air Force recommends that land uses that attract birds not be located near installations with an active air operations mission. These land uses include but are not limited to:....Retention Basins.” Section 5.3 HAZARDS TO AIRCRAFT FLIGHT ZONES (HAFZ), PAGE 5-13.

Appendix B of the SPA is the chart listing the General Plan/Policy and the argument for Consistency of this land use with the goals and policy.

Goal 6, on page vi (page 73 of the SPA pdf file) reads; “Support the continued Military Mission of March Air Reserve Base, and preservation of the airfield from incompatible land use encroachment.” The response is that the design incorporates uses defined in the AICUZ, ALP and DoDI. **However, the 2018 AICUZ specifically recommends against development that requires retention basins.** The March ALUCP incorporates the policies of the AICUZ.

**B. LIGHT EMISSIONS FROM THE PROPOSED PROJECT CREATE A HAZARD TO AIRCRAFT FLIGHT ZONES (HAFZ)**

**“Light Emissions:** Bright lights, either direct or reflected, in the airfield vicinity can impair a pilot’s vision, especially at night....Specific examples of light emissions that can interfere with the safety of nearby aviation operations include:

....

**The increasing use of energy efficient LED lights also poses potential conflicts in areas where pilots use Night Vision Goggles (NVGs), NVGs can exaggerate the brightness of these lights, interfering with pilot vision. (2018 AICUZ Sec. 5.3, Page5-12, 5-13.)**

**“The primary mission of the U.S. Air Force Reserve Command (AFRC) 452 AMW is to provide airlift support for the USAF and to train in tactical airlift and airdrop of personnel and supplies in combat, air refueling, and aeromedical evacuations.” (2018 AICUZ Sec., 2.3, page 2-6, emphasis added.)**

The lighting plan for VIP 215 states **“Lighting shall consist of High Pressure Sodium or LED fixtures (below 2500 Kelvin). (Kelvin is a measure of the color of the light.) (SPA Sec. 3.1.9, Page 3-3, (page 40 of the SPA pdf file.)**

**Lighting and Security** is also explained in Sec. 3.3.2, the SPA General Design Standards.

**“1. Site lighting shall be low or high-pressure sodium, maximum 750-watt, full cut-off fixtures, with the maximum light fixture height of 25 feet above finished grade, and a maximum lighting level of 0.5 foot candles at the property line. For LED lighting an equivalent level shall be provided.” (SPA Page 3-7. Page 44 of the SPA pdf file.)**

The primary mission at MARCH ARB includes training pilots to take off and land with NVGs. The VIP 215 site will be located 950 feet from the landing threshold of the main runway with rows of bright lights on an around the building. Light reflecting off the building walls and concrete parking area which may impair the pilots vision at night when training with NVGs is inconsistent with the AICUZ and the MARCH ALUCP.

C. **THE RCALUC SHOULD ANALYZE IF THE SECURITY MEASURES PROPOSED FOR THE VIP 215 PROJECT MEET THE NEEDS OF CIVIL AND MILITARY AVIATION AT MARCH ARB/ILP.**

The VIP 215 SPA includes section 4.2.5 **Security Elements** states in part:

**“Cameras ...**

No Cameras may be oriented towards the runway and cameras must not record base airfield operations.”

**“Fencing**

Along the runway/airport boundary on the Specific Plan area's eastern edge, special security fencing shall be used. Fencing must be a minimum eight feet in height with three strands of barbed wire. This fencing shall be of a durable material (may be chain link) subject to JPA and March Air Reserve Base review.”

**“Gating...**

Pedestrian and vehicular access gates visible from public areas (i.e. parking lots, streets, sidewalks, etc.) shall be constructed of a durable material, such as tubular steel.”

(SPA page 4-5, page 50 of the SPA pdf file.)

**The SPA overlay adding the warehouse/logistics use has the effect of moving the VIP 215 property outside the Airport Operation Area (AOA). Anyone who has flown since September 2001 has experienced the difference in security outside the AOA from being in the “Secured Area” inside the AOA. Moving the property outside the AOA will impact the “orderly development of air transportation” by the military and civilian aviation at the joint use airport. Carrying on the military mission also includes supporting the Anti-terrorism, Force Protection needs of March ARB.**

Loaded tractor trailers moving into the VIP 215 warehouse area will be within 950 feet of the main runway but outside the AOA secured area. They will not be inspected or questioned by Air Force Security Forces or the Transportation Security Administration (TSA) personnel. (The condition that the tenant lease simply allows U.S. Air Force Security Forces to inspect the property is not a substitute for being inside the AOA and may not be enforceable without a search warrant.)

The RCALUC staff do not think a chain link fence is a sufficient security measure for this Two Million Square Foot warehouse/logistics project. A Level I threat is typically an enemy agent or terrorist seeking to conduct espionage, sabotage or subversion. A Level II threat could include small-scale, irregular forces conducting unconventional warfare. Tractor trailers are much larger than the Ryder rental truck used to bomb the Alfred P. Murrah Federal Building in Oklahoma City on April 19, 1995. The close proximity to the main runway of such a large project makes the added security risk prohibitive to the safe operation of March.

**D. THE RCALUC, MJPA AND SURROUNDING JURISDICTIONS HAVE IMPLEMENTED THE MARCH ALUCP HARMONIOUSLY WITH THE AICUZ.**

In 2017, technical inconsistencies between the 2014 MARCH ALUCP and the Department of Defense Instructions (DoDI) became apparent.

An unrelated project was proposed in the Airport Protection Zone 1 for a textile manufacturing facility. The ALUC interpreting MARCH ALUCP Appendix 4, a list of non-recommended activities, held this textile manufacturing facility was consistent with the March ALUCP. But Textile Manufacturing is prohibited in APZ 1 by DoDI 4165.57 (Table 1) highlighting an inconsistency between the MARCH ALUCP and military operating requirements of the joint use airport. The RCALUC now uses the DoDI list of prohibited uses for projects in the APZs.

The ALUCP method of calculating intensity of use in the APZ1 and APZ 2 in table MA-2 is inconsistent with DoDI 4165.57 Table 1, Note 1. The RCALUC, local communities and the MJPA have used interim measures to harmonize their development standards with the DoDI, pending an update to the MARCH ALUCP.

The MARCH ALUCP does not specifically provide for the Clear Zone, APZ 1 and APZ 2 of Runway 12-30. These areas are almost entirely on the March ARB property. These issues will be studied in the pending Compatible Use Study (CUS).

**3. SUMMARY OF THE FACTS REGARDING THE HISTORY AND CURRENT STUDIES, MARCH ARB/INLAND PORT ALUCP AND THE VIP 215 PROJECT:**

This Supplement to the Staff Report is intended to add context to facts relevant to the history of the MARCH ALUCP and the VIP 15 PROJECT.

**A. The FAA March Airport Layout Plan- includes a west side taxiway which could be connected to provide access to the runway from the subject property.**

The VIP 215 property General Plan designation was Aviation (AV): "Through joint use of the aviation field, the unique opportunity to develop civilian aviation is presented. Land uses under this designation include flightline, hangars, aviation support services, inclusive of fuel systems and dispensing, air cargo storage, passenger and air cargo terminals, fixed base operations, aircraft maintenance and aviation operation. ..."

2005 March ARB completed an AICUZ Study.

2010 March ARB and the local communities completed a Joint Land Use Study (JLUS). The subject property is within the B2 Zone described in the JLUS as:

"Beneath or adjacent to final approach and initial departure flight corridors or

**adjacent to the runway. Not within Accident Potential Zones.” JLUS Exhibit 3-2. (Emphasis added.)**

The JLUS became the draft basis for the 2014 March ARB/Inland Port Airport Land Use Compatibility Plan (MARCH ALUCP)

2014 March Air Reserve Base/ Inland Port Airport Land Use Compatibility Plan (MARCH ALUCP) was adopted by the RCALUC.

**“The MARCH ARB/IPA ALUCP maintains the established format (from other Riverside County ALUCP’s). Thus, only the policies and maps specific to March ARB/IPA for insertion into Chapter 3 and the background data to be added to Volume 2 are presented here. All the countywide policies in Chapter 2 of Volume 1 are considered to be part of the March ALUCP unless explicitly modified or supplemented by the March Specific policies.” (MARCH ALUCP, Overview.)**

MARCH ALUCP “is primarily based” upon the U.S. Air Force AICUZ 2005. (2014 MARCH ALUCP, Sec. MA.1,1.1). “The airfield consists of two runways. The primary runway (14-3) - oriented north-northwest/south-southwest- is 13,300 feet in length and is the longest runway open to civilian use in the state. The second smaller runway, Runway 12-30, is just over 3000 feet....” (2014 MARCH ALUCP, Sec. MA.1,.2)

December 2016 the MJPA submitted plans and documents to the RCALUC for the proposed warehouse/logistics center, Veterans Industrial Park 215 (VIP215) on the subject property. The Applicant, Riverside Inland Development, LLC, worked with MJPA Staff to revise the project for over a year and a half. (8/8/2018 Revised Project Submittal to RCALUC file No. ZAP1274MA17).

February 2018 the March ARB issued an updated AICUZ.

June 2018 Matthew J. Burger, Col. USAF, Commander March ARB supported a Joint Land Use Study (JLUS).

**“2. A JLUS is a tool used to analyze impacts by operations of a military installation on local jurisdictions as well as land use impacts to operations on a military installation. ”**

**“3. Certain land uses and conditions, as well as operations by the United States Air Force (USAF) and Air Force Reserve Command (AFRC) in the vicinity of MARB should be studied to help safeguard the military mission and protect the health, safety and welfare of the public.”**

**“4. Areas of study that are of interest to the USAF and AFRC mission include, but are not limited to: 1.) population densities in the vicinity of MARB; 2.) rising**

ground water conditions; 3.) storm water and flooding potential caused by development around the installation; 4.) study of clear zones/accident potential zones for Runway 12-30; 5.) landscaping potential to attract wildlife causing flight safety impacts; 6.) increased glare from passive reflective roof surfaces and solar panel/photovoltaic development; and 7.) mitigating land use and traffic impacts within clear zones.”

A copy of Col. Burger’s letter is attached as Exhibit 1 to this Staff Report Supplement.

August 2018 the MJPA revised its submission of the VIP 215 for the Specific Plan Overlay, the Plot Plan and Tentative Parcel Map for Two approximately One Million Square Foot Warehouse/Logistics buildings. The U.S. Air Force raised many concerns about that project.

October 11, 2018 the RCALUC by a rare 4-3 vote found the VIP 215 project, with the AV(SP-8) Specific Plan Overlay, allowing general warehousing/logistics uses and a plot plan for two one million square foot warehouses on the subject property **CONSISTENT** with the MARCH ALUCP. That form of the VIP 215 project has not been adopted by the March Joint Powers Commission.

December 2019 representatives of the local jurisdictions, the March Joint Powers Authority, March ARB and other locally impacted agencies met with a representative of the Federal Office of Economic Assistance (OEA) to apply for a grant to assist with the cost of the JLUS, now called a Compatible Use Study (CUS).

February 2020 the current VIP 215 project to revise the Plot Plan and Tentative Parcel Map for **the into a single Two Million Square Foot Building** was submitted to RCALUC. In keeping with the RCALUC current policy, the staff requested a Bird Aircraft Strike Hazard study (BASH). The BASH study suggested changes to the Specific Plan. The application was revised to include amendments to the Specific Plan SP-8 overlay. The BASH study initially submitted suggested **considering moving the retention basins** from the east side of the project to the west side as one method to mitigate the BASH impact. The applicant submitted a revised BASH Study removing that possible mitigation measure.

The U.S. Air Force has renewed its many concerns about impacts of the VIP 215 Project on the military operation. The Air Force provided RCALUC Staff with its comments on the VIP 215 proposed EIR. In addition to other issues it raised the need to comply with the National Environmental Protection Act (NEPA) because the Specific Plan storm water interim proposal includes directing water onto the Base.

In June 2020 the Office of Economic Assistance approved a grant for 90% of the \$630,000 cost of conducting the Compatible Use Study (CUS) of the impacts of development around March ARB, including but not limited to the impacts raised by Col. Burger, many of which manifested themselves after the 2010 JLUS and the 2014 MARCH ALUCP.

The issues raised by the VIP 215 project, such as security, drainage and glare, among others will be considered in the upcoming Compatible Use Study (CUS) requested by the Air Force and supported by the March JPA, the RCALUC and other local jurisdictions. The CUS will study issues to protect the long term viability of March ARB. Due to the VIP 215 project's size and close proximity to the runway, it could impact the CUS review process or ability to implement its conclusions. As an example the CUS will look at the impact on military operations of increased glare from passive reflective roof surfaces, such as the two million square feet of roof surface on this proposed project.

4. **TO COORDINATE PLANNING SO AS TO PROVIDE FOR THE ORDERLY DEVELOPMENT OF AIR TRANSPORTATION INCLUDES ADVANCING AND PROTECTING THE LONG TERM VIABILITY OF THE MARCH AIR RESERVE BASE AND ITS PUBLIC AND PRIVATE SECTOR USES.**

The mission of the RCALUC is sometimes summarized as protecting people from airports and airports from people. Protecting the long term viability of March ARB requires understanding the Base Realignment and Closure (BRAC) process. This issue was included in a MJPA Technical Advisory Committee agenda item on August 22, 2016. Attached is a portion of the staff report prepared August 18, 2016 discussing the BRAC process.

As of 2016 the Department of Defense and the President had taken all necessary actions to precipitate the next BRAC round. However, after five repeated requests, Congress has not authorized a new BRAC round. (August 18, 2016 Memo, page number 190)

The memo offers insight into the BRAC Selection Criteria and the needs at March ARB. Stormwater is specifically identified as a continuing concern and references the VIP 215 property identified as Parcel D-2.

“Directly west of the main MARB runway is an undersized natural-lined drainage channel known as the “little Suez”. The little Suez currently collects tributary flows from approximately 3,000 acres west of the main runway as it traverses through March Inland Port's parcel D-2, crosses MARB boundaries and runs parallel along the main runway before it terminates at the Riverside County Flood Control District's Lateral B south of MARB. Due to the facility's proximity to the main artery of MARB's operations, the strategic planning and coordination of drainage improvements for properties west of the I-215 freeway and parcel D-2 are of utmost importance.” (August 18, 2016 staff memo, page 192.)

Stormwater continues to be a concern and is included in the CUS. Concerns raised by the U.S. Air Force with ALUC staff and comments on the EIR for the VIP 215 project on parcel D-2 are detailed in the Staff report. The U.S. Air Force has indicated that this project is critical to resolve this long standing stormwater concerns. The U.S. Air Force has requested that the interested parties and jurisdictions reach a final agreement to an acceptable solution to the stormwater



impact on the Base, including a written agreement committing funding its implementation. As part of that the U.S. Air Force has also requested a written agreement for implementation and funding of the interim solution which will flow the storm water over the base until the final solution is constructed.

Another need of the March ARB referenced in this 2016 memo which continued to today are Utilities: Water and Gas, which are included in the Compatible Use Study.

In 2016 another current need was to upgrade Runway 14/32 to support very large military aircraft. "MARB's Runway 14/32 is measured at 13,300 feet in length, one of the longest airport runways west of the Mississippi, making MARB an attractive asset for aircraft operations." Rising ground water and storm water impacts, including from the VIP 215 project may jeopardize the basing of MAC aircraft at March. These issues are included in the CUS.

##### 5. CONCLUSION:

The RCALUS should consider if the necessary agreements, commitments land rights have been obtained to carry out the necessary conditions to meet the impacts and provide for the orderly development of military and civilian air transportation at this airport.

Staff recommends that the Commission CONTINUE the matter to the August 13, 2020 meeting pending completion of the Air Force review of the project *and that the Air Force is satisfied that their concerns have been addressed. If the applicant will not consent to the continuance the Staff must recommend a finding of INCONSISTENT based on the cumulative impacts of this large scale development in its entirety.*

- A. Two Million Square Foot warehouse plus additional hard surfaced parking impacts.
- B. Drainage retention basins attracting Bird Aircraft Strike Hazard (BASH).
- C. Visual roof glare only 950 feet from the runway impairing pilot vision.
- D. Light emissions interfering with pilot Night Vision Goggle training.
- E. Security risk - loaded tractor trailers 950 feet from the runway but outside the AOA.
- F. Circumventing the Compatible Use Study (formerly JLUS).
- G. Not protecting the long term viability of March ARB from future BRAC.

Respectfully submitted:

July 1, 2020



Simon A. Housman, Director  
Riverside County Airport Land Use Commission.



DEPARTMENT OF THE AIR FORCE  
AIR FORCE RESERVE COMMAND



1 June 2018

MEMORANDUM FOR LOCAL GOVERNMENTS IN THE VICINITY OF MARCH ARB

FROM: 452 AMW/CC  
2145 Graeber Street, Suite 117  
March ARB CA 92518-1667

SUBJECT: Position on Joint Land Use Study by RCALUC

1. March Air Reserve Base (MARB) personnel are supportive of the Riverside County Airport Land Use Commission (RCALUC) pursuing and undertaking a Joint Land Use Study (JLUS).
2. A JLUS is a tool used to analyze impacts by operations of a military installation on local jurisdictions as well as land use impacts to operations on a military installation. It is a cooperative study to help provide a policy framework to support adoption and implementation of compatible development regulations near a military base.
3. Certain land uses and conditions, as well as operations by the United States Air Force (USAF) and Air Force Reserve Command (AFRC) in the vicinity of MARB, should be studied to help safeguard the military mission and protect the health, safety and welfare of the public.
4. Areas of study that are of interest to the USAF and AFRC mission include, but are not limited to: 1.) population densities in the vicinity of MARB; 2.) rising ground water conditions; 3.) storm water and flooding potential caused by development around the installation; 4.) study of clear zones/accident potential zones for Runway 12-30; 5.) landscaping potential to attract wildlife causing flight safety impacts; 6.) increased glare from passive reflective roof surfaces and solar panel/photovoltaic development; and 7.) mitigating land use and traffic impacts within clear zones.
5. The MARB Final Air Installation Compatibility Use Zone Study (AICUZ) was released to the public on February 21, 2018 to all surrounding jurisdictions, to include the Cities of Riverside, Moreno Valley, Perris; County of Riverside; and March Joint Powers Authority. The last time this document was updated was in 2005, which was used in part as the basis of the Riverside County's Airport Land Use Commission's March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan (ALUCP). Any JLUS effort going forward should use the AICUZ as a launching-off point for continued discussion and collaboration, and updating of the ALUCP.

6. Updating the ALUCP without conducting a JLUS could potentially cause key impacts to not be analyzed and studied. It is MARB personnel's desire to have as much cooperation and collaboration with local land use authorities to ensure the health, safety and welfare of the public and USAF members alike, as well as help sustain the military mission.

7. Based on the above-stated conditions we recommend that a JLUS study be conducted prior to the adoption of an updated ALUCP.

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Date: 2018.06.13 09:48:08 -0700  
MATTHEW J BURGER, Col, USAF  
Commander

## EXECUTIVE SUMMARY

In this report, we discuss some of the current and future needs of the March Air Reserve Base. RAND Corporation has developed a framework for creating and managing public-private partnerships for protecting and advancing military bases. Staff recommends using this framework for addressing the Base's needs. In order to perform these functions without subsidies from its member agencies, the Authority will need an independent source of operating revenue. Operating an airport is also a resource-intensive enterprise, which requires collaboration among many stakeholders, including the elected leaders of the Authority's member agencies. A divestment of the Authority's land use authority, which is currently its main source of operating revenue, before the Airport can sustain itself and support the Base, will require a commensurate increase in financial resources.

However, if the Airport development projects are completed, staff estimates that the Authority would have the resources to refocus its mission on protecting and advancing the base, and overseeing the public and private sector airport uses at the March Inland Port. Staff's analysis of the March Inland Port's sustainability is contained under Tab (2).

In order to transfer territories to the member cities, the Riverside Local Agency Formation Commission ("LAFCo") must perform a municipal services review ("MSR") and sphere of influence amendments. Staff recommends starting this process around the time the Authority executes individual leases for newly developed parcels at Veterans Industrial Park 215 ("VIP 215"). Once completed, VIP 215 and the general aviation facilities at Parcel D-1 have the potential to generate enough revenue for the Authority to protect and advance the March Air Reserve Base, and oversee the public and private sector airport uses at the March Inland Port.

### SECTION 1. PROTECTING AND ADVANCING THE MARCH AIR RESERVE BASE.

The March Joint Powers Authority's mission is to bring good jobs to Riverside County. In order to do this, the Authority must protect and advance the March Air Reserve Base. The Base and the civilian side of the Airport have key roles in the Inland Empire's long-term economic development. By developing a civilian air cargo center and developing complementary employment opportunities in the neighboring areas, the Authority has the potential to create an industrial center for more than 32,000 jobs. Staff estimates that the active development projects currently subject to the Authority's land use authority will yield more than 26,000 jobs by 2040.

Pursuant to the Authority's acquisition agreement with the United States Government, all revenue derived from airport properties must be reinvested into the airport. An independent airport authority would fund government affairs and public engagement if the airport is to operate effectively. This would include educating the public on uses for the base, buying property around the airport periphery to prevent encroachment, processing claims, handling accidents and collisions, and negotiating with neighboring property owners and jurisdictions regarding compatible property uses.

BRAC

The long term viability of March Air Reserve Base is predicated on its ability to survive the next BRAC round. The DoD and the President may request a BRAC round; however, if Congress does not fund the action, a BRAC round will not commence. Figure 1 is a flowchart depicting the steps necessary to lay the foundation *before* a BRAC round may be convened. To date, the Department of Defense and the President have taken all necessary actions to precipitate the next BRAC round. However, after five repeated requests, Congress has not authorized a new BRAC round. Due to budget constraints, it is anticipated the Department of Defense will continue to request a BRAC round authorization.

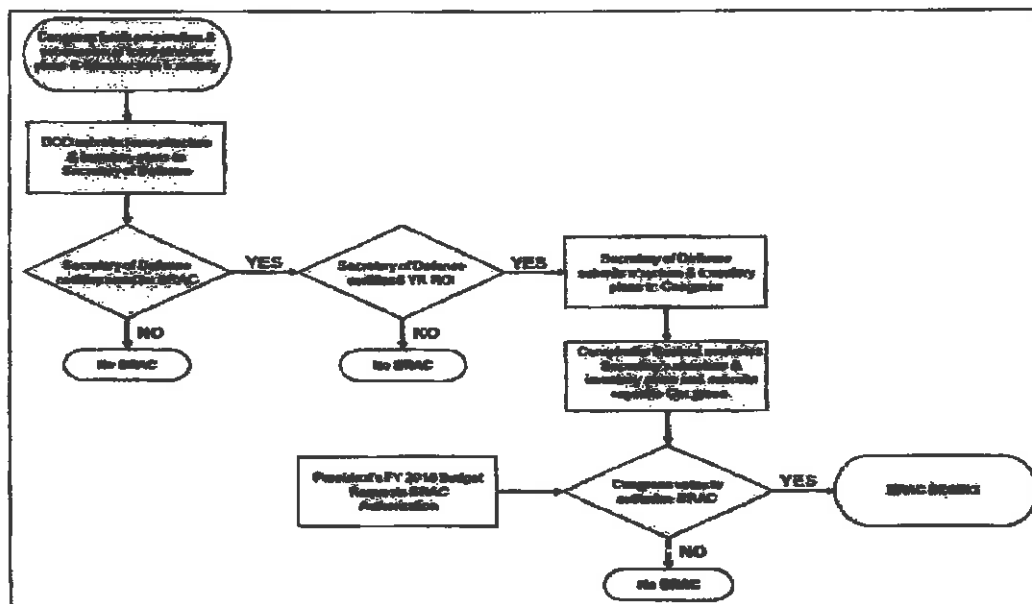


Figure 1. Pre-BRAC authorization approval flowchart. Adapted from National Defense Authorization Act for Fiscal Year 2015, H.R. 4435, 113th Cong., 2014.

### 1. BRAC Selection Criteria

Any discussion about refining the Authority's mission to protect the long term viability of MARB should focus on mitigating risks associated with BRAC selection criteria. Enabling legislation for each BRAC round identified three distinct selection categories: (a) military value, (b) return on investment, and (c) impacts to the economy, infrastructure, and environment. These three broad categories were the basis of selection for the Secretary of Defense's list of recommendations to the BRAC Commission. Of these selection criteria, military value has consistently been the driving factor for determining whether a military installation will be selected for closure or realignment. Each of the five BRAC rounds identified military value as the highest priority for considering whether to close or realign an installation.

The 1988 BRAC round's definition of military value is identified in Table 1 below.

Table 1  
*Military Value Factors and Attributes*

Factor	Attributes
Mission suitability	Site-specific mission Deployment means Relationship to other activities Weather/terrain/land use Survivability Maneuver space
Availability of facilities	Operations Support Infrastructure Administration
Quality of facilities	Condition Technology Configuration
Quality of life	Family housing Bachelor housing Recreation/amenities Medical
Community support	Workforce Commercial transport Infrastructure Complementary industry

For the 1991, 1993, and 1995 BRAC rounds, the military value definition was further refined resulting in the following four elements:

1. **Current and future mission requirements:** The force structure report identified current and future staffing levels for all military branches. Based on these requirements, military bases were evaluated to determine their impact on the operational readiness of the total force structure.
2. **Infrastructure:** Land, buildings, and airspace were assessed to ascertain their condition and availability to support the DoD's projected force structure. Both existing installations and potential receiving installations were assessed.
3. **Responsiveness:** Consideration for responsiveness to force structure contingencies was a factor in selecting military installations for realignment or closure. Additionally, the ability to respond to mobilization efforts at existing and potential receiving military bases was considered in the selection process.

4. **Costs:** The costs associated with implementing these changes and the manpower associated with the proposed force structure were considered in assessing the military value of installations.

For the 2005 BRAC round, the selection criteria were modified to emphasize the DoD's desire to transform the military and foster a concept of "jointness." In modifying the criteria, an emphasis was placed on creating or maintaining joint training and joint command facilities. For example, current and future mission requirements were modified to include the impact "on joint war fighting, training and readiness." The definition of infrastructure was expanded to include "training areas suitable for maneuver by ground, naval, or air forces throughout a diversity of climate and terrain areas and staging areas for the use of the Armed Forces in homeland defense missions." Lastly, the 2005 BRAC round military value selection criteria were modified to ensure that any shift in the need for additional staffing levels could be accommodated quickly. The DoD's ability to respond to shifts in staffing necessitated revising the responsiveness criterion to identify surge capabilities as an important factor comprising military value.

## ***2. Current Needs at March Air Reserve Base***

In light of the previous BRAC rounds selection criteria's emphasis on military value, any refinement of the Authority's mission should include an emphasis on mitigating the following threats to MARB's military value.

### ***a. Stormwater***

Over the past eight years MARB has expressed continued concerns regarding stormwater impacts on existing Base operations. The natural lined Heacock and Cactus Channels are an immediate threat to MARB security measures. A partnership effort between local jurisdictions and MARB resulted in the scheduled construction of Heacock Channel Fall 2016. A similar effort will need to be replicated on the Cactus Channel, which currently sits under the flight path and directly impacts the main entrance to MARB during storm events.

Directly west of the main MARB runway is an undersized natural-lined drainage channel known as the "little Suez". The little Suez currently collects tributary flows from approximately 3,000 acres west of the main runway as it traverses through March Inland Port's parcel D-2, crosses MARB boundaries and runs parallel along the main runway before it terminates at the Riverside County Flood Control District's Lateral B south of MARB. Due to the facility's proximity to the main artery of MARB's operations, the strategic planning and coordination of drainage improvements for properties west of the I-215 freeway and parcel D-2 are of utmost importance.

### ***b. Utilities: Water and Gas***

MARB currently operates on water and gas infrastructure that date back to the 1940's. While MARB operations were minimized under the 1993 BRAC round, upgrades to systems within surrounding communities expanded as vacant properties were occupied by new developments that installed water and gas infrastructure that met incumbent utility codes. In recent years, MARB's mission evolved as DoD's efforts on homeland security and defense

evolved, resulting in the need to have reliable backbone infrastructure that will support MARB's ability to quickly respond to the DoD's mission-based needs. This means that water and gas utilities that directly tie into MARB's infrastructure must be upgraded to meet incumbent utility codes, and to ensure that developments around MARB are not undermining the MARB's ability to maintain adequate fire and gas flow onsite.

*c. Airport Runway 14/32*

MARB's Runway 14/32 is measured at 13,300 feet in length, one of the longest airport runways west of the Mississippi making MARB an attractive asset for aircraft operations. The runway, however, requires upgrades in certain areas that will allow MARB to support very large military aircraft. March Inland Port Airport (MIP) has access to MARB's runway through a Joint Use Agreement, and currently contributes to the overall function of the flying facilities by improving and maintaining those runway extensions and taxiways that are within the MIP Authority's control. Continued, and enhanced, advocacy efforts are needed in order to attract federal funds to MARB and MIP.

**3. March Inland Port Airport**

Pursuant to the Authority's Public Benefit Conveyance from the United States Government, all revenue derived from airport properties must be reinvested into the airport. An independent airport authority would have to fund government affairs and public engagement if the airport is to operate effectively. This would include educating the public on uses for the base, buying property around the airport periphery to prevent encroachment, processing claims, handling accidents and collisions, and negotiating with neighboring property owners and jurisdictions regarding compatible property uses.

A divestment of the Authority's land use authority, which is currently the Airport Authority's main source of operating revenue, will require a commensurate increase in financial resources. The Green Acres housing community currently generates approximately \$300,000 in rent revenue per year, and could help sustain the Airport Authority.

*a. Personnel*

As land use compatibility becomes an ongoing matter of concern for MARB and surrounding communities, it has become increasingly important for local planners to obtain feedback on development issues from MARB Civil Engineering (CE) staff. However, as MARB's staffing needs are dependent upon the availability of federal funds, maintaining a single point of contact within MARB CE for community planning purposes can be a challenge. The Authority can assist with MARB's community planning needs, by expanding current interactions with the Base CE's office on proposed developments not only within the Authority's jurisdiction, but developments within neighboring jurisdictions that are within the airport's influence area.

*b. RAND Study*

In addition to aforementioned support actions by the Authority, the following discussion offers additional strategies that would contribute to the advancement of March ARB's viability in the region. RAND Corporation (2016) issued a study *Military Installation Public-to-Public*



*Partnerships Lessons from Past and Current Experiences* that outlined the value, barriers and cost-effectiveness of public-public partnerships. The study found these partnerships offer military bases and their host communities with a wide range of benefits, including reducing or avoiding costs, improving services, accessing specialized equipment and capabilities and improving community-installation cooperation. The study further identified ten possible benefits that can be achieved through public-public partnerships:

1. Improved military mission.
2. Economic benefits, including cost savings, earnings and cost avoidance.
3. Improved installation and community operations, facilities, infrastructure, workforce and services.
4. Access to additional capacity in resources, skills, expertise, facilities, and infrastructure.
5. Improved strategic regional collaboration.
6. Improved government and community relationships.
7. Enhanced outreach to military personnel and their families and communities.
8. Energy and environmental benefits.
9. Facilitator and political help with federal, state, and local governments and other organizations.
10. Helping maintain community character and way of life.

Currently, the Authority participates with MARB to: secure additional missions and enhance existing missions; improve the installations infrastructure; facilitate regional collaboration; and facilitate political objectives. Any reorganization of the Authority would need to continue and expand upon current partnerships with MARB.

#### *c. Community Centric Public-Public Partnerships*

Creating strategic public-public partnerships is an opportunity for the Airport Authority to protect the long term viability March Air Reserve Base. Numerous examples of public-public partnerships dedicated to advancing the interests of military installations exist across the United States. For example:

- **Travis Community Consortium:** Comprised of member agencies that include Solano County, Solano Community College, Travis Credit Union, Solano EDC, Travis Regional Armed Forces Committee, and the cities of Fairfield, Suisun, and Dixie. Each agency shares in the cost of advocacy services for Travis Air Force Base. The Consortium adopted a 4 year 8-point Strategic Plan to: strengthen and enhance

partnerships and joint ventures; implement policy that will ensure continued compatible regional development; and seek to preserve other existing missions and enhance the potential to assume additional missions. The entire Strategic Plan is an attachment to this report. The Travis Community Consortium 2014-2018 Strategy is included under Tab (3).

- **Fort Leonard Wood Community Partnership:** Provides a forum for local leaders to pursue ideas and concepts for new partnerships. Members include the garrison commander, neighboring communities and civic organizations. The group's members advise on organization policies and capabilities, as well as provide insight into how their organization could support a partnership opportunity. The partnership committee's efforts have resulted in successful repair work on a state road that runs through the installation and a memorandum of agreement that assigned maintenance responsibilities for said road. Other initiatives in the works include partnerships to provide animal control, visitor center support, landscaping services and renewable energy.
- **Greater Oklahoma City Chamber Coalition:** State and local officials along with Air Force leaders broke ground last month on the new KC-46A Tanker Sustainment Campus at Tinker Air Force Base, a community-led initiative that will allow a key economic engine for the region to expand. The 158-acre facility will be the home of maintenance, repair and overhaul operations for the new aerial refueling tanker and be part of the Oklahoma City Air Logistics Complex. The land, formerly owned by the Burlington Northern and Santa Fe Railway Co. was purchased in February 2015 through a public-public partnership. The Air Force contributed \$8 million toward the purchase price, Oklahoma City contributed \$23.5 million and Oklahoma County provided \$12.5 million.

#### ***4. Summary***

As previously discussed, the advancement and protection of the long term viability of March Air Reserve Base and its public and private sector uses, must consider existing threats related to a BRAC round. Supporting MARB's needs for infrastructure, airfield improvements and personnel is of utmost importance.

**Decision Point:**      *How will the Airport Authority be funded to undertake the mandate of advancing and protecting the long term viability of March Air Reserve Base and its public and private sector uses?*

## Rull, Paul

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**From:** WATERS, DOUGLAS S GS-13 USAF AFRC 452 MSG/CE <douglas.waters.2@us.af.mil>  
**Sent:** Thursday, May 14, 2020 3:49 PM  
**To:** Rull, Paul; Pacino, Brian  
**Cc:** SHAW, DAVID N Maj USAF AFRC 452 MSG/CD; Housman, Simon; Guerin, John  
**Subject:** RE: MARB BCE\_Comments for ALUC May 14 Commission Meeting  
**Attachments:** Response draft EIR VIP 215 Development.pdf

Raul,

We can discuss further next week, however, next week is already full and we need to tie a time down now. I would suggest 10:30 Monday.

I offer you the attached comments concerning the draft EIR and the following comments that have been directed to the March JPA as well.

The March ARB position on the VI 215 development remains, as stated by Brig Gen Coburn, that a signed agreement as to funding and schedule by all parties is required before March ARB can concur with the VIP 215 development. In fact at the last meeting on this topic Both Brig Gen Coburn and my notes indicate that the Riverside County Flood Control District (RCFLCD) would come back to us with a detailed plan on the underground option for the long term Lateral B project. We also wrote that we could expect an MOU in the next 30-60 days to be signed by all parties to document an agreement and at that time the TAC would also approve the project.

To date RCFLCD has not sent this plan to us. Until we see an acceptable plan, we don't even know what the long term plan looks like and if March ARB can be a party. Though, as Brig Gen Coburn indicated in the meeting, an underground option is possible.

Doug Waters, PE, CEM,  
Chief Engineering Flight / Deputy BCE  
452 MSG/CE  
US Air Force Reserve Command  
610 Meyer Dr., Bldg 2403  
March ARB, CA 92518-2188

Douglas.waters.2@us.af.mil  
Office- 951-655-4852/4851  
Cell- 928-304-2451  
DSN- 447-4852

**From:** Rull, Paul <PRull@RIVCO.ORG>  
**Sent:** Thursday, May 14, 2020 2:48 PM  
**To:** Pacino, Brian <Brian.Pacino@jacobs.com>  
**Cc:** WATERS, DOUGLAS S GS-13 USAF AFRC 452 MSG/CE <douglas.waters.2@us.af.mil>; SHAW, DAVID N Maj USAF AFRC 452 MSG/CD <david.shaw.5@us.af.mil>; Housman, Simon <shousman@rivco.org>; Guerin, John <JGUERIN@RIVCO.ORG>  
**Subject:** [Non-DoD Source] RE: MARB BCE\_Comments for ALUC May 14 Commission Meeting  
**Importance:** High

Good Afternoon Brian,



**DEPARTMENT OF THE AIR  
FORCE  
AIR FORCE RESERVE COMMAND**

March 11, 2020

452 Civil Engineer Squadron  
610 Meyer Drive, Building 2403  
March Air Reserve Base, CA 92518

Mr. Jeffrey Smith  
Planner  
1455 Meridian Parkway, Suite  
140  
Riverside, CA 92518

Dear Mr. Smith,

This is in response to the draft VETERANS INDUSTRIAL PARK 215 PROJECT, Environmental Impact Report, prepared by March Joint Powers Authority (MJP), March 2020 in accordance with California Environmental Quality Act (CEQA) Guidelines Section 15123.

Based on the project presented in the DEIR, March ARB (MARB) contends that the project is both a state and Federal action and therefore requests that MJP submit AF form 813 to the Base Civil Engineer, MARB, describing the full scope of the project, so that MARB may review the project and determine the level of environmental assessment that is required under the National Environmental Protection Act of 1972 and current Council for Environmental Quality Guidance. NEPA requires that any action on Federal Lands must follow the environmental assessment process of NEPA and 32 Code of Federal Regulations 989 provides guidance on the Environmental Impact Assessment Process (EIAP) to be followed when an action is an Air Force action or is on Air Force real property. It is recommended that both CEQA and NEPA be conducted concurrently or sequentially following Federal and then State guidelines.

The project described in the DEIR makes it clear that there is a portion of the project that is on March ARB property. Paragraph ES.2 of the DEIR indicates there is an onsite portion of the project which encompasses approximately 142.5 acres located within the jurisdiction of the March JPA and an offsite portions of the project located within March JPA, City of Perris, and MARB the extent of which is not described. In fact, MARB is aware of both the drainage outfall planned as part of the project as a short term solution to the stormwater flooding issues from the contributing basin and also a long term project to transport the stormwater off of MARB. This long term plan has not been developed substantially enough to show how it would affect MARB. The last proposal from the Riverside County Flood Control District would take

all the waters from this project and carry them in an underground channel on MARB real property for a substantial distance. Other proposals have not been acceptable.

Based on these two portions of the project that are clearly necessary and part of the proposed action, March requires that the environmental assessment follow NEPA. As such MARB would be the lead agency for the NEPA EIAP. When this project was first proposed and until February 2020, it was understood that the long term project would not require MARB real property and therefore it was felt that a short term drainage issue may be addressed through a categorical exclusion under 32 CFR 989 guidance.

Brigadier General Melissa A. Coburn, Commander, 452 Air Mobility Wing, stated in her letter of July 2, 2019, that MARB's primary concern around the VIP 215 development is stormwater runoff affecting Runway 14-32. She further stated that MARB is looking to a result that addresses the ultimate drainage solution to this area rather than any interim solution.

Brig Gen Coburn stated that funding and implementation of the regional storm water project is the main solution to the flooding problems that affect March ARB and our concerns over adjacent developments. These efforts require extensive environmental review, land acquisition, and the securing of easements, etc. and can take substantial time to accomplish. She made it clear that, before she could agree to an interim solution, there must be a signed agreement concerning schedule and funding by all parties for completion of the regional project. To date this long term solution has not been presented to MARB.

Brig Gen Coburn stated that the project will require compliance with the National Environmental Protection Act. Air Force regulations requires the proponent conduct or fund the EIAP necessary to allow a decision by the Commander concerning any significant impacts from the action.

It was MARB intention to utilize on-gong environmental assessments, either concerning the implementation of actions proposed in the Installation Natural Resource Management Plan or of a new military construction project proposed by the Air Force, to base our analysis on the portion of the project short term stormwater outfall on AF real property. She emphasized that the approach to be taken depends upon review by Air Force Reserve Command headquarters and MARB Staff Judge Advocate, as well as the potential to delay decisions on those very important actions.

In addition the DEIR raises several concerns and issue that must be addressed in any environmental assessment and which do not appear to be adequately addressed in this DEIR. These issues and concerns are described in the attachment.

**draft VETERANS INDUSTRIAL PARK 215 PROJECT EIR**

**Should you have any questions or would like to discuss this further, please feel free to contact me directly, Major David Shaw, Base Civil Engineer at (951) 655-4851 or myself, (951) 655-4852.**

**WATERS.DOUGLA** Digitally signed by  
**S.STUART.JR.1261** WATERS.DOUGLAS.STUART.J  
**479148** RN,1261479148  
Date: 2020.08.11 16:03:22 -0700

**DOUGLAS S WATERS, JR. PE**  
**Deputy Base Civil Engineer and Acting Chief**  
**Environmental Flight**

**CC:**

**David Shaw, Base Civil Engineer**

**Attachment:**

**JPA VIP 215 DEIR Comments**

**ATTACHMENT 1  
JPA VIP 215 DEIR  
Comments**

**Natural Resources**

**ES-17 and**

**ES-19, Impact 3.3-4**

1. There is no mention or discussion of the MARB burrowing owl populations and habitats. Species do not stop at a fence, there is a direct connection between actions that affect the burrowing owl on one side of the fence and the habitat on the other. There is an annual nesting site right on the border/fence of this land that needs to be addressed. This project will have a direct effect on this site and annual nesting.

**ES-19, Impact 3.3-4**

**ES-19, Impact 3.3-5**

The document claims there will be no Cumulative effects.

1. There will be cumulative affects due to encroachment of habitat of the burrowing owl. Habitat in nearby lands now developed by March Joint Power Authority have already been reduced over the years leaving MARB and the few border lands as remaining habitat. The loss of habitat outside MARB drives the birds to the land s and habitat inside MARB. This becomes a significant BASH issue as MARB attempts to control the habitat and the potential for aircraft mishaps due to increased bird populations. This project will further reduce habitat.

**2-20, 2.3.10 Landscaping**

1. Landscaping should be limited to native species or cultivar species approved by Cal-IPS not known to be invasive.

**3.3-29, SKR HCP,**

1. Just because we have no HCP doesn't mean you dont analyze the effects for SKR. There is a lot of missing current data. No one has checked MARB records for SKR information. MARB has current surveys and a new report (2020) that will help with the analysis of SKR, burrowing owls and vernal pools/ fairy shrimp in relation to your project.
2. SKR has been extirpated from the west side of the freeway
3. All drainage offsite from project area contributes to the drainage which is known to have fairy shrimp. However, in the 2019 surveys and 2020 report, these drainages were deemed not suitable habitat due to scouring flows washed through the channel system.
4. Burrowing owls will be affected by the actions of this project. Especially cumulatively! Request review of the past populations, current and foreseeable future of the land use to discuss the cumulative loss of habitat for burrowing owls. This project is directly adding

to the loss of habitat and with the future of more buildings being proposed by MJPA throughout this area, there re definitely cumulative impacts!

### **3.3-31 Literature Review.**

1. Include the latest survey reports. Available at MARB for SKR, BUOW and vernal pools/fairy shrimp. Contact Chris Wagner, Natural Resources Manager 951-655-3653

## **Cultural Resources**

### **3.4-8**

1. Twenty-Nine Palms Band of Mission Indians was not notified or consulted with, according to this documentation

### **3.4-11**

1. Where are the DPR forms mentioned?
2. It was mentioned in the document that March ARB was contacted for information. MARB was never contacted about the natural and cultural resources.

### **3.4-12**

1. Where is this 1966 aerial?

### **3.4-15**

1. All historic and cultural resources were evaluated through the BRAC of the base. Did you read the BRAC EA and confirm that these are not in fact listed? The agreement for BRAC was that the responsibility of all listed historic resources would be managed by JPA. I see no reference to this document

### **3.4-22**

1. MARB is interested in the historic findings on this land would like to make sure it is properly evaluated under "significance of a historical resource' that is either listed or eligible for listing in the National Register". This will require further studies in past historic information on the buildings discussed here.. 1616, 1617, 1622. MARB is interested in ensuring all history of these sites are recorded for the historic legacy of this base. At the time of BRAC, these structures may have had a history in the cold war that were not yet 50 years or they may have been missed in full DPR history.
2. The document claims that there was extensive research but there is no mention of documents including DPR forms and BRAC EA to prove this was analyzed thoroughly. We would like further studies on the historic significance of these structures. MARB



would like to see the new DPR forms and we would like to ensure that proper consultation with SHPO if needed, was done!

3. No one consulted with the cultural resources manager on MARB to determine if we have historic information on these facilities!
4. MARB feels that there is not enough evidence to claim that these structures did not meet any of the criteria because not all documents were researched. Again there is no reference that the BRAC EA or past DPR forms for these structures was assessed.

**3.4-23**

1. The MARB Cultural Resource manager is interested in being consulted on these structures, including seeing these structures and taking photos.

**3.4-25**

1. The document claims that these structures did not meet the NRHP or CRHR Criteria for historic resources. MARB feels that until all resources are researched properly, the historic significance of these structures is still in question. We request that all resources available be researched and consult the MARB cultural resource manager.

**Rull, Paul**

**From:** Pacino, Brian <Brian.Pacino@jacobs.com>  
**Sent:** Wednesday, May 13, 2020 2:35 PM  
**To:** Rull, Paul  
**Cc:** 'WATERS, DOUGLAS S GS-13 USAF AFRC 452 MSG/CE'; 'SHAW, DAVID N Maj USAF AFRC 452 MSG/CD'  
**Subject:** MARB BCE\_Comments for ALUC May 14 Commission Meeting

Paul,

On behalf of MARB Base Civil Engineering, please see following comments for your meeting tomorrow. Understand the objection comment tied to ZAP1405MA20 may be FYI for said meeting but wanted to include it so you are in the loop as to current stance for Base on that proposed project as it concerns March JPA. Appreciate ALUC's due diligence on the applicable staff report findings and incorporating BASH concerns.

Let us know if you have any questions otherwise.

V/r,

Brian  
CTR, 452 MSG/CE

(on behalf of Maj David Shaw and Mr. Doug Waters, 452 MSG/CE)

**FOR OFFICIAL USE ONLY:**

ALUC Case#	Development Title	Rooftop Solar?	ALUC Zone	Comments
1. ZAP1400MA20	Placentia Logistics Warehouse	Yes	C2 (Not in APZs)	<ul style="list-style-type: none"><li>Since the sites are located in Zones C1 &amp; C2, we recommend confirmation from ALUC that project application will be subject to FAA/OES analysis to determine maximum allowable building height.</li><li>Concur with ForgeSolar PASS findings for proposed rooftop solar, however we support analysis of cumulative impacts on airfield operations as part of upcoming Compatible Use Study in conjunction with the OEA.</li></ul>
2. ZAP1404MA20	Perris Warehouse	Yes	C1 (Not in APZs)	
3. ZAP1405MA20	VIP-215 Warehouse <i>*Overlaps with March JPA Draft EIR, Proposed Plot Plan and Tentative Parcel Map, and latest</i>	No	B2 (Not in APZs)	<ul style="list-style-type: none"><li>Concur with April 9, 2020 ALUC staff report findings (received via e-mail on 3/20/20) citing previous MARB concerns (via AF letters in Sept. 2016 and 2018) raised for security, drainage, glare,</li></ul>

	<i>interim drainage channel improvement plan</i>			<ul style="list-style-type: none"> <li>◦ safety/HAZMAT storage, and Aviation land use/zoning.</li> <li>◦ Concur with latest redlined Specific Plan report (received via e-mail on 4/23/20) that developer shall comply with previously identified BASH concerns.</li> <li>◦ <b>However:</b> 1) MARB has responded with comment to March JPA concerning Draft EIR that NEPA will be required for Developer's proposed interim drainage channel solution, and 2) MARB cannot approve any proposed long term drainage channel solution on behalf of Riverside County Flood Control until NEPA is complete for the proposed interim solution.</li> </ul>
4. ZAP1406MA20	Meridian Sharp Warehouse+Office	No	B2 (Not in APZs)	<ul style="list-style-type: none"> <li>◦ Project is located outside MARB airfield restriction zones (CZ, APZ, etc.) but nearest proposed building is roughly one mile WNW of Runway 14 north end, and is just 1,500 feet west of Runway 14 APZ I.</li> <li>◦ Per latest MARB 2018 AICUZ, the site is located in the 60db CNEL noise contour. Developer needs to provide more input on proposed use of the two buildings in relation to sound attenuation.</li> <li>◦ Since the site is located in Zone B2 (High Noise Zone), we recommend confirmation from RC ALUC that project application will be subject to FAA/OES analysis to determine maximum allowable building height and other potential air navigation hazards.</li> <li>◦ No anticipated stormwater or utility issues for MARB, as the project is contained within the existing Meridian Business Park and well west of the I-215 freeway.</li> </ul>
5. ZAP1411MA20	21600 Cactus Ave.	Yes	B1, B2, C1 (Not in APZs)	<ul style="list-style-type: none"> <li>◦ Since the site is located in both Zones B1 Inner Approach/Departure), B2 (High Noise) and C1 (Primary Approach/Departure), we recommend confirmation from RC</li> </ul>

				<ul style="list-style-type: none"> <li>• IUC that project application will be subject to FAA/OES analysis to determine maximum allowable building height.</li> <li>• Concur with ForgeSolar PASS findings for proposed rooftop solar, however we support analysis of cumulative impacts on airfield operations as part of upcoming Compatible Use Study in conjunction with the OEA.</li> </ul>
6. ZAP1412MA20	Senior Living Riverside (old AF Village West)	No	C2 (Not in APZs)	<ul style="list-style-type: none"> <li>• No comment/objection</li> </ul>

**Brian J. Pacino, AICP | Jacobs | Buildings, Infrastructure & Advanced Facilities |**  
 949.224.7835 office | 703.627.3010 mobile | [brian.pacino@jacobs.com](mailto:brian.pacino@jacobs.com) | [www.jacobs.com](http://www.jacobs.com)

# NOTICE

**THERE IS AN AIRPORT NEARBY.**

**THIS STORM WATER BASIN IS DESIGNED TO HOLD  
STORM WATER FOR ONLY 48 HOURS AND  
NOT TO ATTRACT BIRDS**

**PROPER MAINTENANCE IS NECESSARY TO AVOID  
BIRD STRIKES**



**IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:**

**Name:** \_\_\_\_\_

**Phone:** \_\_\_\_\_

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Mail Processing Center  
 Federal Aviation Administration  
 Southwest Regional Office  
 Obstruction Evaluation Group  
 10101 Hillwood Parkway  
 Fort Worth, TX 76177

Aeronautical Study No.  
 2020-AWP-644-OE  
 Prior Study No.  
 2016-AWP-12029-OE

Issued Date: 02/27/2020

Kathy Hoffer  
 Riverside Inland Development, LLC  
 901 Via Piemonte  
 Suite 125  
 Ontario, CA 91764

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building Vetrans Industrial Park I-215  
 Location: Ferris, CA  
 Latitude: 33-52-47.00N NAD 83  
 Longitude: 117-15-50.81W  
 Heights: 1521 feet site elevation (SE)  
 49 feet above ground level (AGL)  
 1570 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 08/27/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

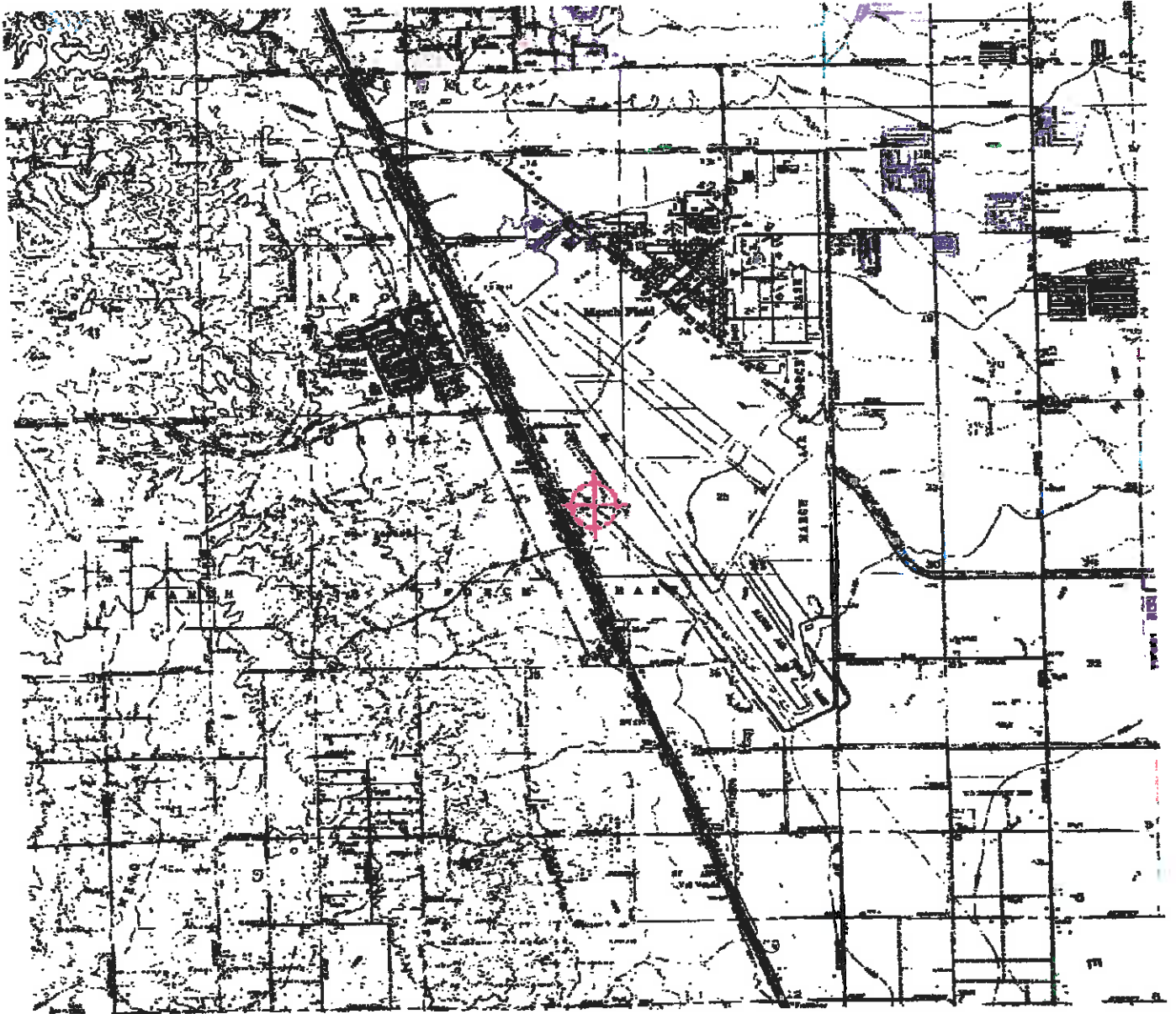
If we can be of further assistance, please contact our office at (206) 231-2990, or [paul.holmquist@faa.gov](mailto:paul.holmquist@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-AWP-644-OE.

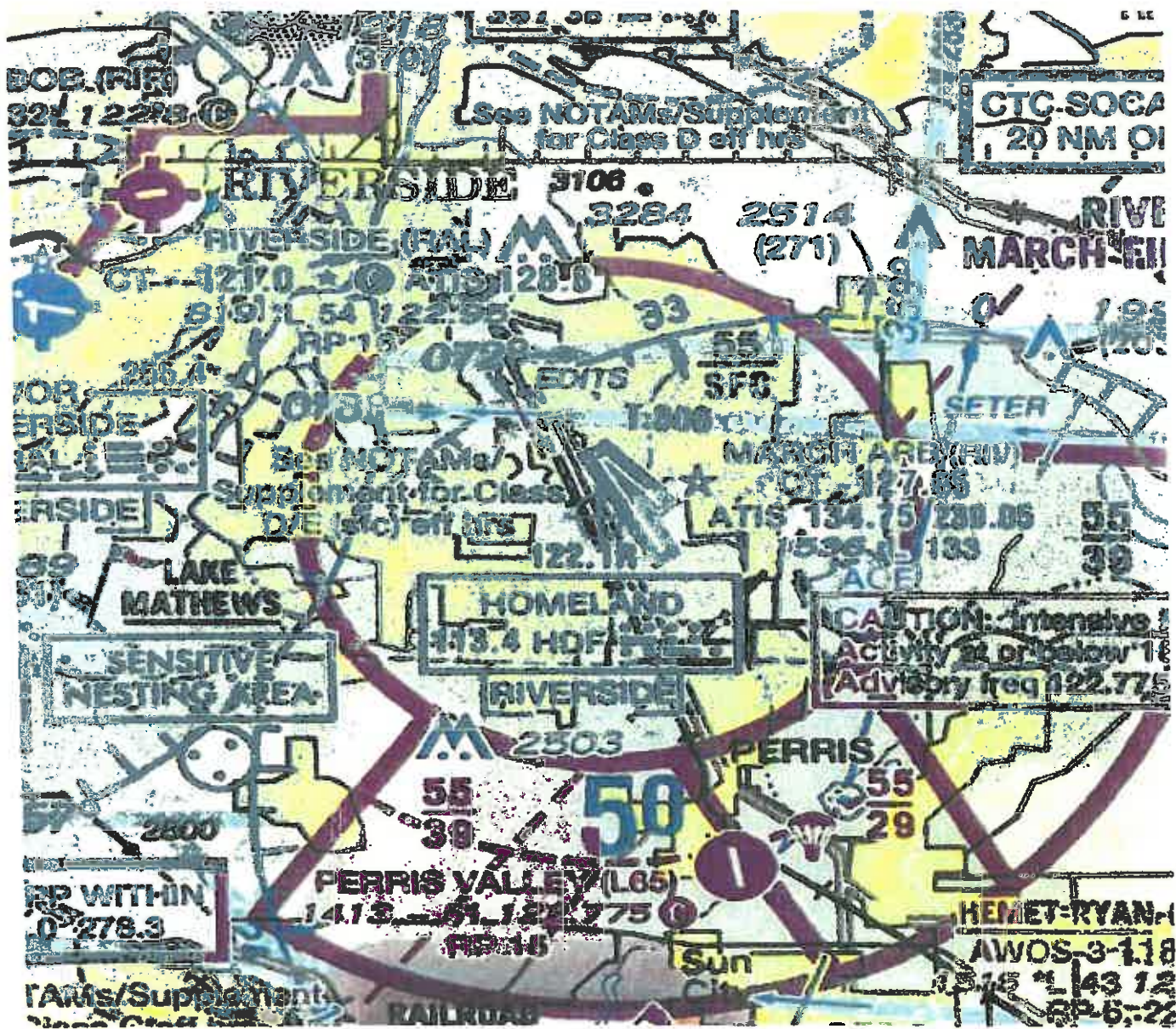
**Signature Control No: 428280706-432020104**  
Paul Holmquist  
Specialist

(DNE)

Attachment(s)  
Map(s)









Mail Processing Center  
 Federal Aviation Administration  
 Southwest Regional Office  
 Obstruction Evaluation Group  
 10101 Hillwood Parkway  
 Fort Worth, TX 76177

Aeronautical Study No.  
 2020-AWP-645-OE  
 Prior Study No.  
 2016-AWP-12029-OE

Issued Date: 02/27/2020

Kathy Hoffer  
 Riverside Inland Development, LLC  
 901 Via Piemonte  
 Suite 125  
 Ontario, CA 91764

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building Vetrans Industrial Park I-215  
 Location: Perris, CA  
 Latitude: 33-52-44.07N NAD 83  
 Longitude: 117-15-56.37W  
 Heights: 1525 feet site elevation (SE)  
 53 feet above ground level (AGL)  
 1578 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 08/27/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

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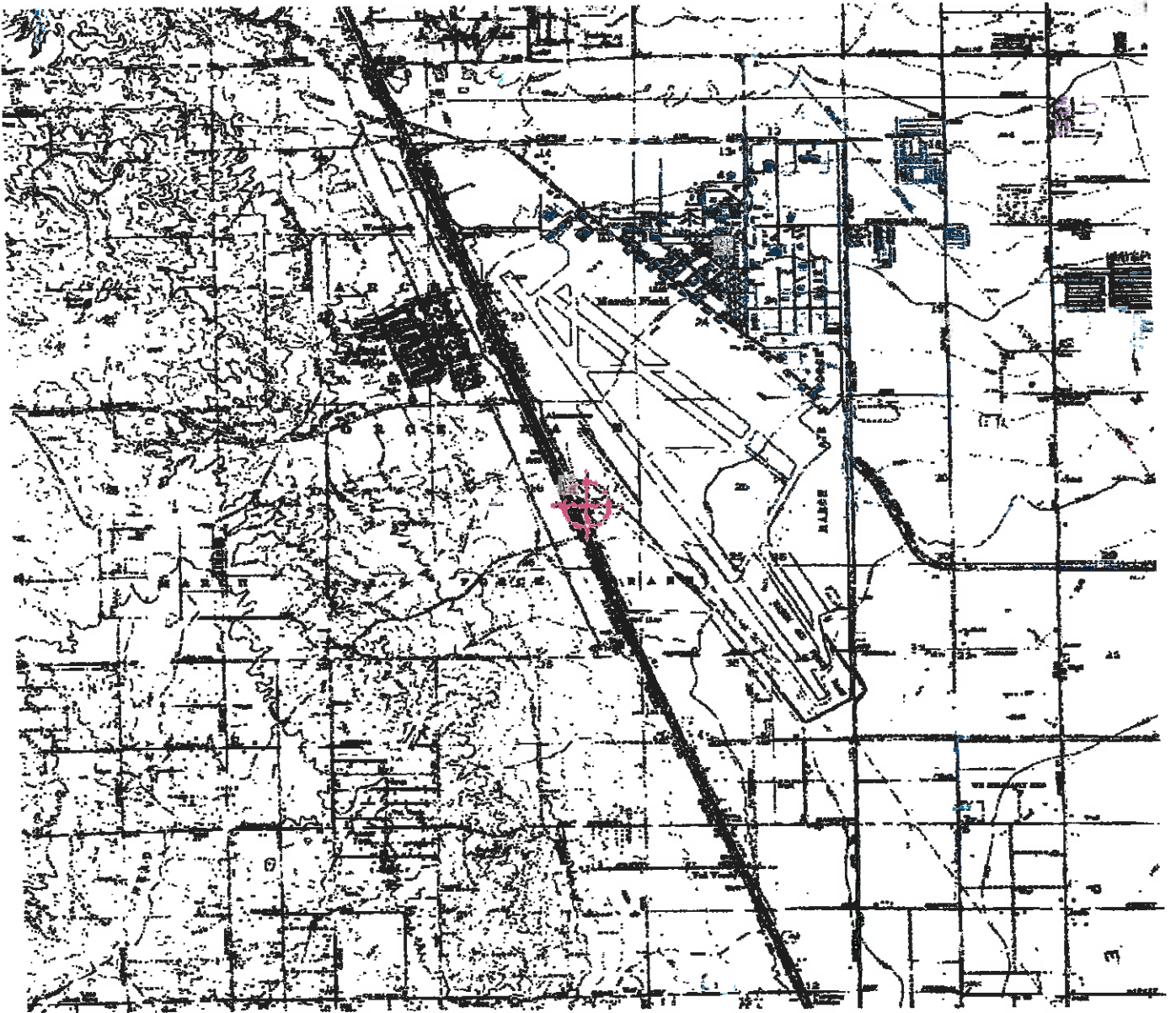
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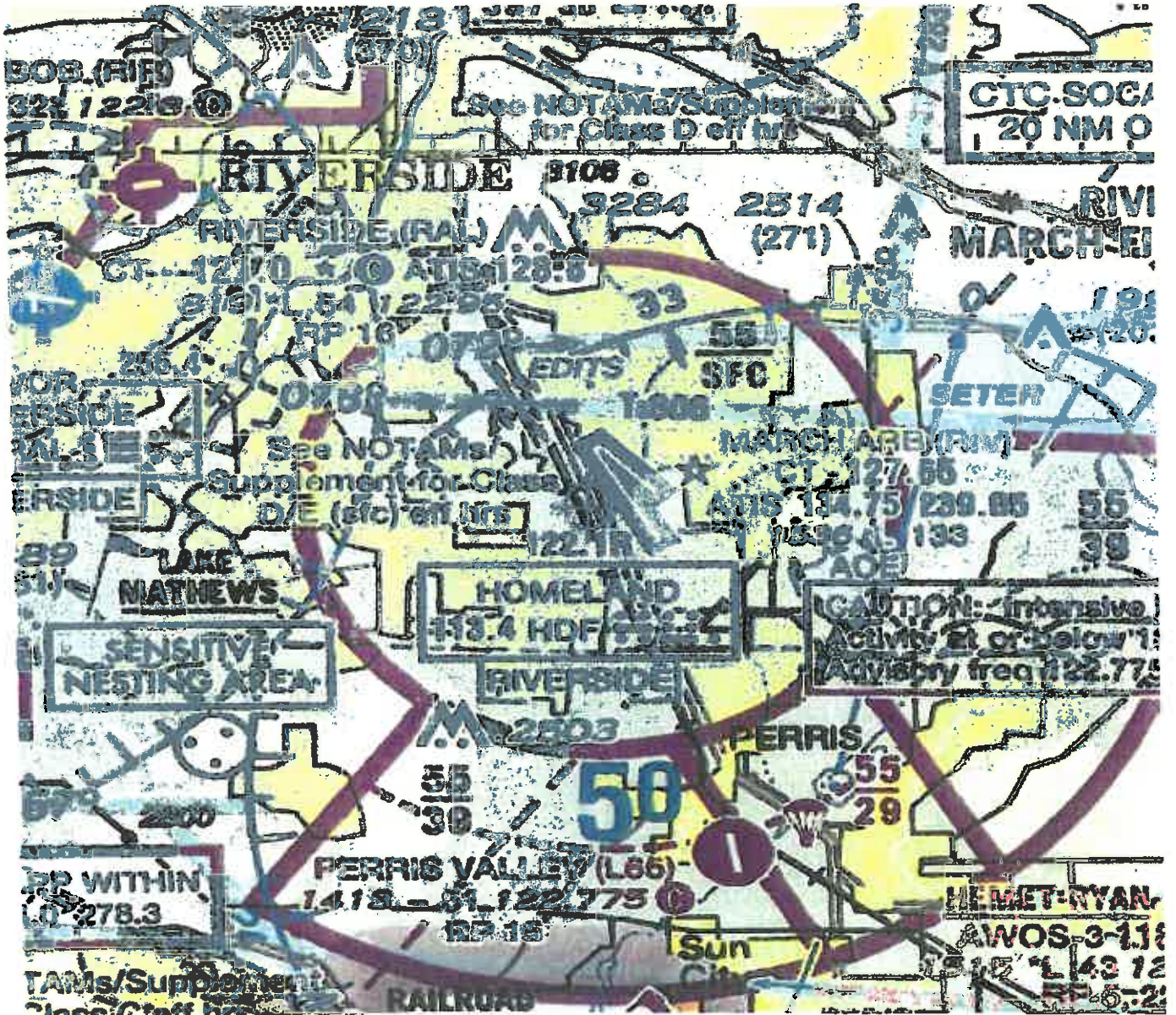
If we can be of further assistance, please contact our office at (206) 231-2990, or [paul.holmquist@faa.gov](mailto:paul.holmquist@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-AWP-645-OE.

**Signature Control No: 428280708-432020108**  
Paul Holmquist  
Specialist

( DNE )

Attachment(s)  
Map(s)







Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2020-AWP-646-OE  
Prior Study No.  
2016-AWP-12029-OE

Issued Date: 02/27/2020

Kathy Hoffer  
Riverside Inland Development, LLC  
901 Via Piemonte  
Suite 125  
Ontario, CA 91764

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Building Veterans Industrial Park I-215
Location:	Perris, CA
Latitude:	33-52-16.96N NAD 83
Longitude:	117-15-29.44W
Heights:	1504 feet site elevation (SE) 48 feet above ground level (AGL) 1552 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

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If we can be of further assistance, please contact our office at (206) 231-2990, or [paul.holmquist@faa.gov](mailto:paul.holmquist@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-AWP-646-OB.

**Signature Control No: 428280710-432020109**

Paul Holmquist  
Specialist

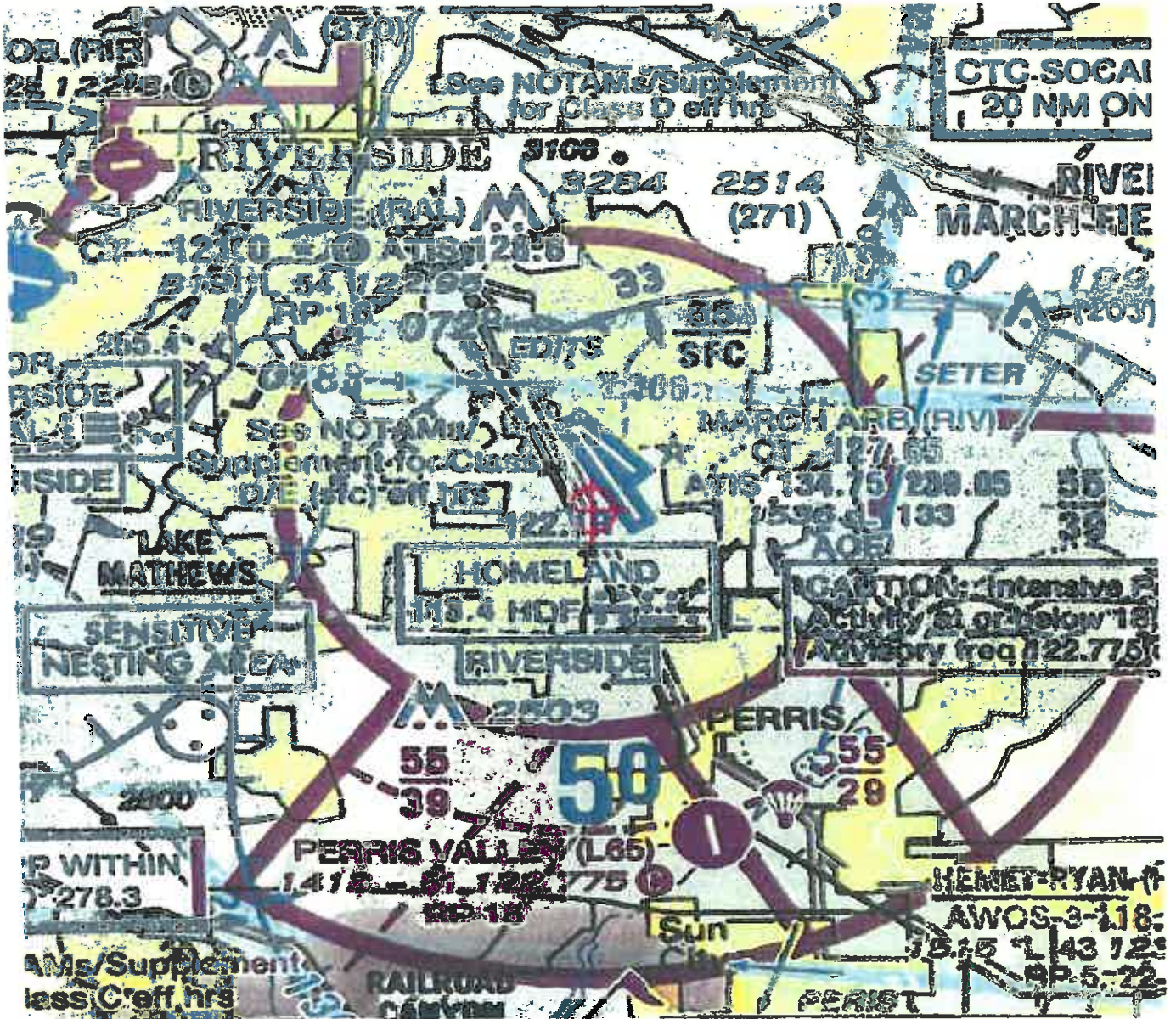
( DNE )

Attachment(s)

Map(s)









Mail Processing Center  
 Federal Aviation Administration  
 Southwest Regional Office  
 Obstruction Evaluation Group  
 10101 Hillwood Parkway  
 Fort Worth, TX 76177

Aeronautical Study No.  
 2020-AWP-647-OE  
 Prior Study No.  
 2016-AWP-12029-OE

Issued Date: 02/27/2020

Kathy Hoffer  
 Riverside Inland Development, LLC  
 901 Via Piemonte  
 Suite 125  
 Ontario, CA 91764

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building Vetrans Industrial Park I-215  
 Location: Perris, CA  
 Latitude: 33-52-14.43N NAD 83  
 Longitude: 117-15-35.39W  
 Heights: 1507 feet site elevation (SE)  
 54 feet above ground level (AGL)  
 1561 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

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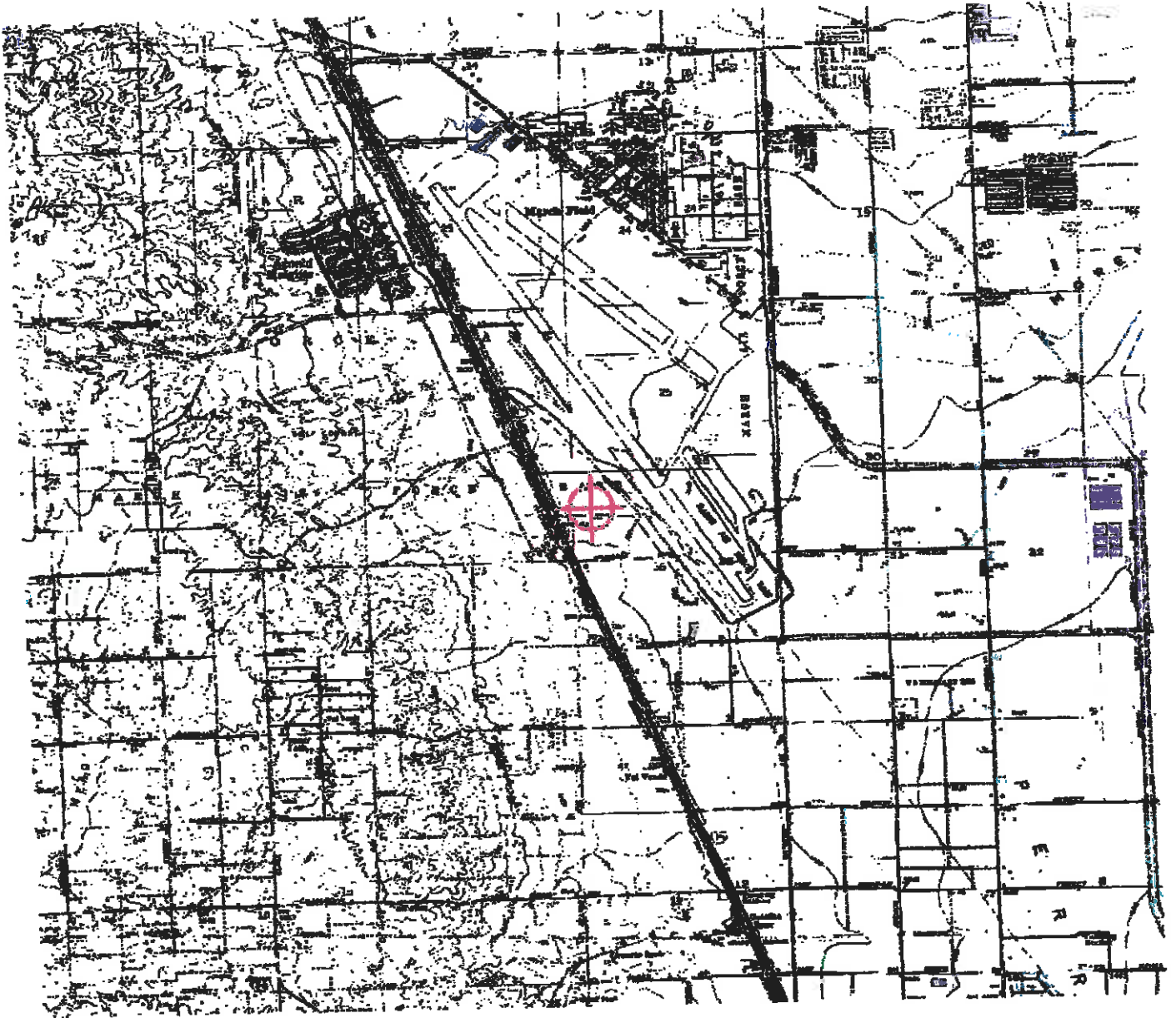
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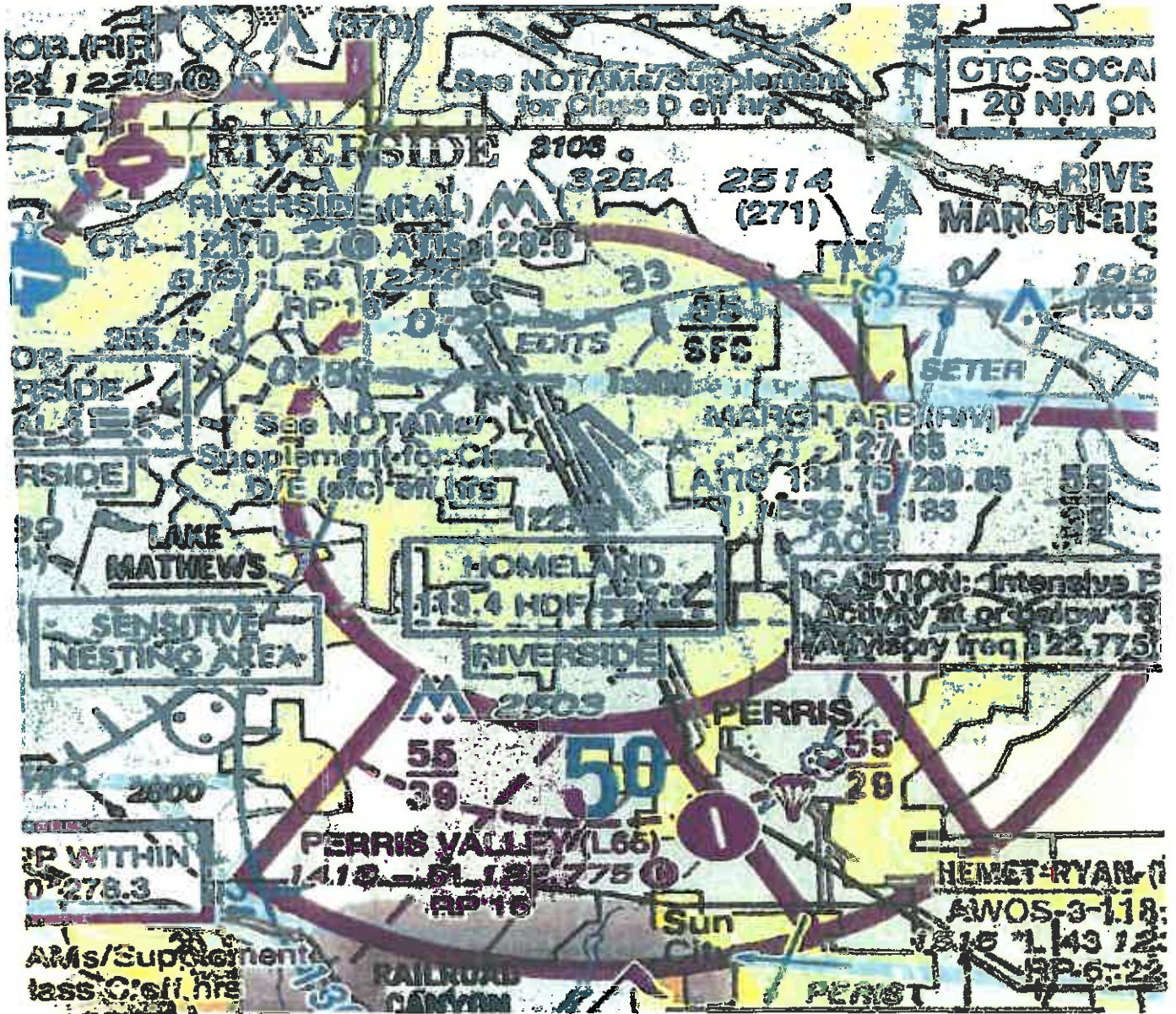
**If we can be of further assistance, please contact our office at (206) 231-2990, or [paul.holmquist@faa.gov](mailto:paul.holmquist@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-AWP-647-OE.**

**Signature Control No: 428280712-432020107**  
**Paul Holmquist**  
**Specialist**

**( DNE )**

**Attachment(s)**  
**Map(s)**







Mail Processing Center  
Federal Aviation Administration  
Southwest Regional Office  
Obstruction Evaluation Group  
10101 Hillwood Parkway  
Fort Worth, TX 76177

Aeronautical Study No.  
2020-AWP-648-OE  
Prior Study No.  
2016-AWP-12029-OE

Issued Date: 02/27/2020

Kathy Hoffer  
Riverside Inland Development, LLC  
901 Via Piemonte  
Suite 125  
Ontario, CA 91764

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building Vetrans Industrial Park I-215  
Location: Perris, CA  
Latitude: 33-52-46.70N NAD 83  
Longitude: 117-15-51.42W  
Heights: 1525 feet site elevation (SE)  
53 feet above ground level (AGL)  
1578 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

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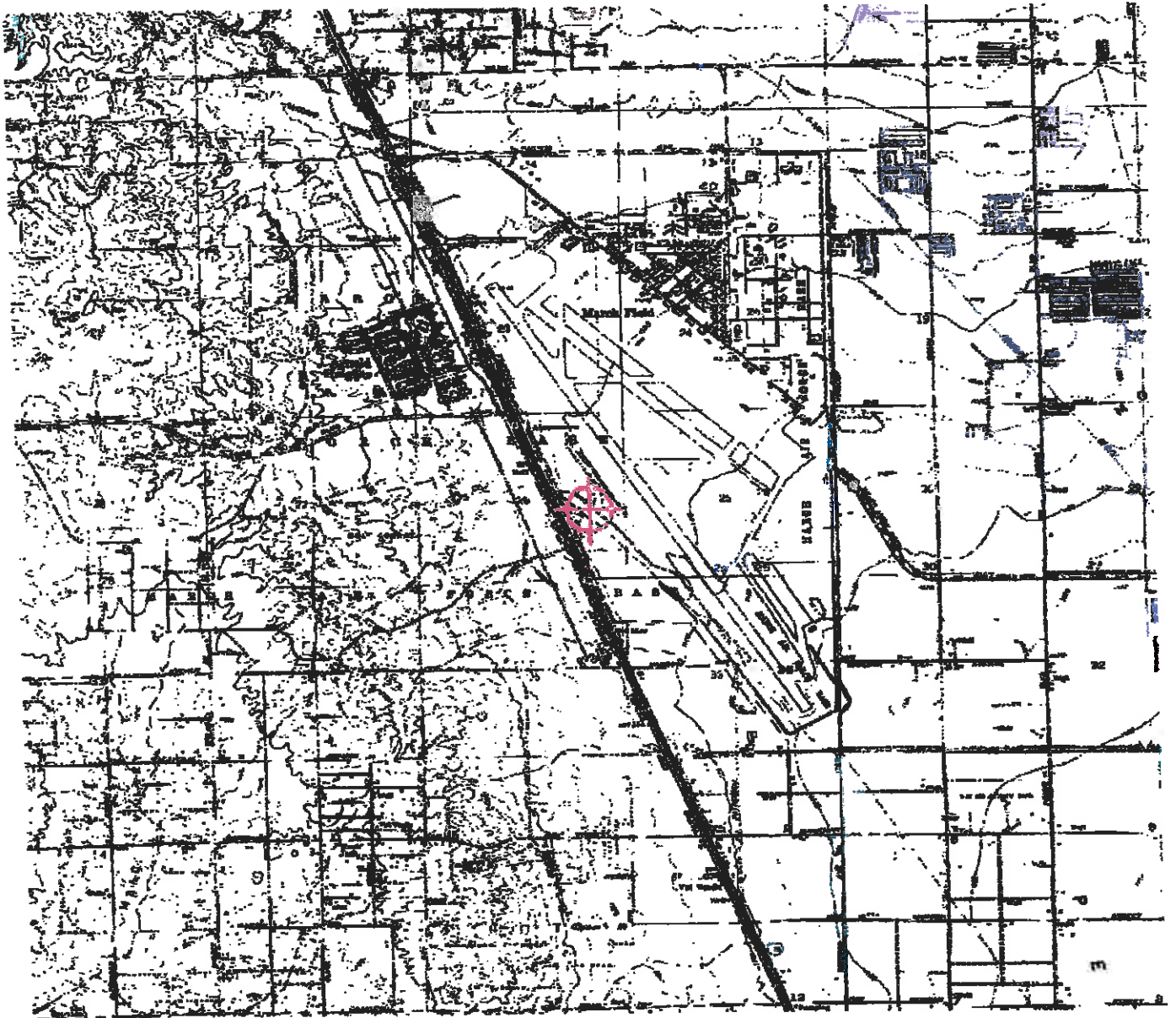
**Signature Control No: 428280714-432020105**

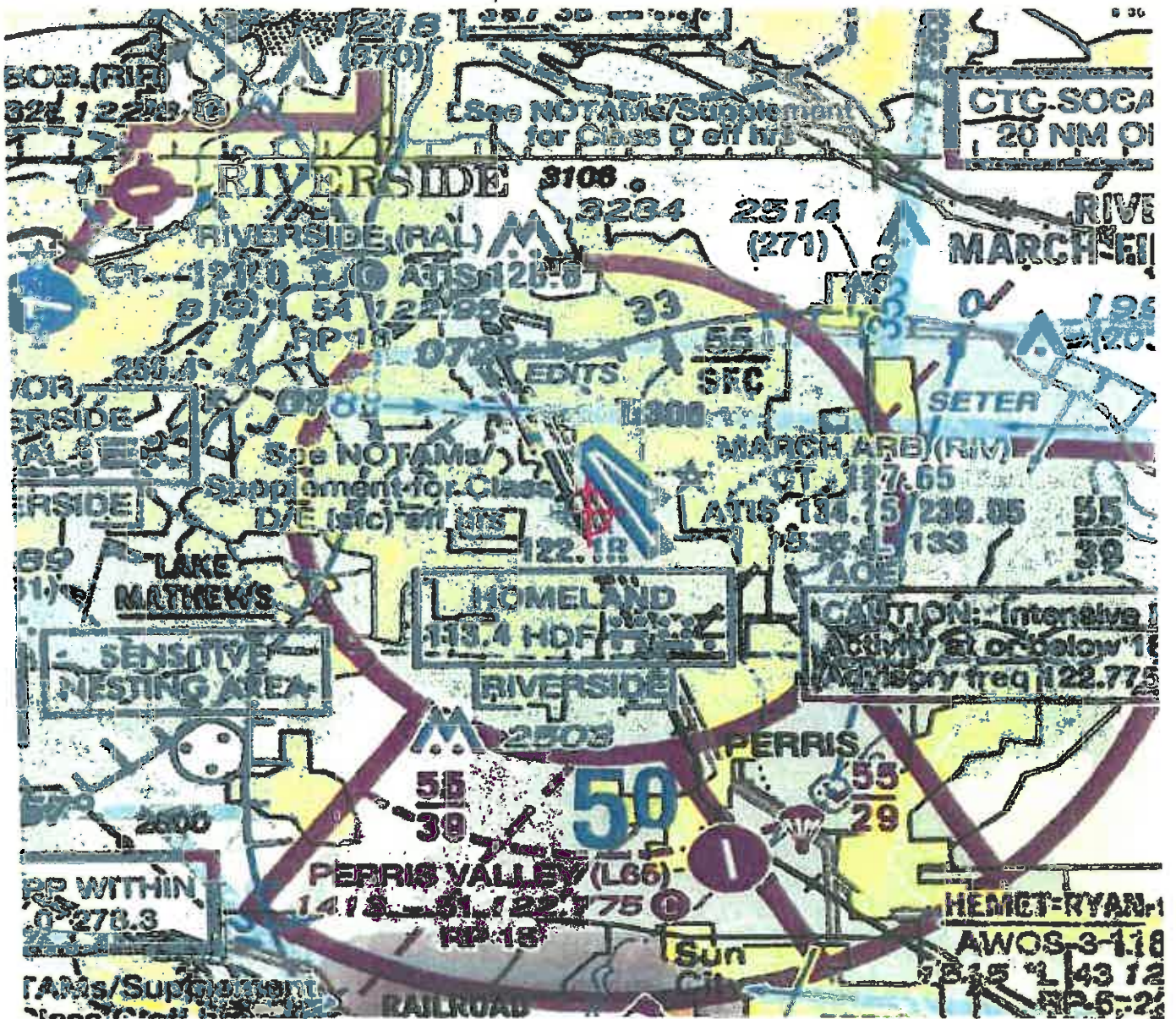
**Paul Holmquist  
Specialist**

**( DNE )**

**Attachment(s)  
Map(s)**









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Kathy Hoffer  
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901 Via Piemonte  
Suite 125  
Ontario, CA 91764

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Structure: Building Vetrans Industrial Park I-215  
Location: Ferris, CA  
Latitude: 33-52-16.66N NAD 83  
Longitude: 117-15-30.06W  
Heights: 1507 feet site elevation (SE)  
53 feet above ground level (AGL)  
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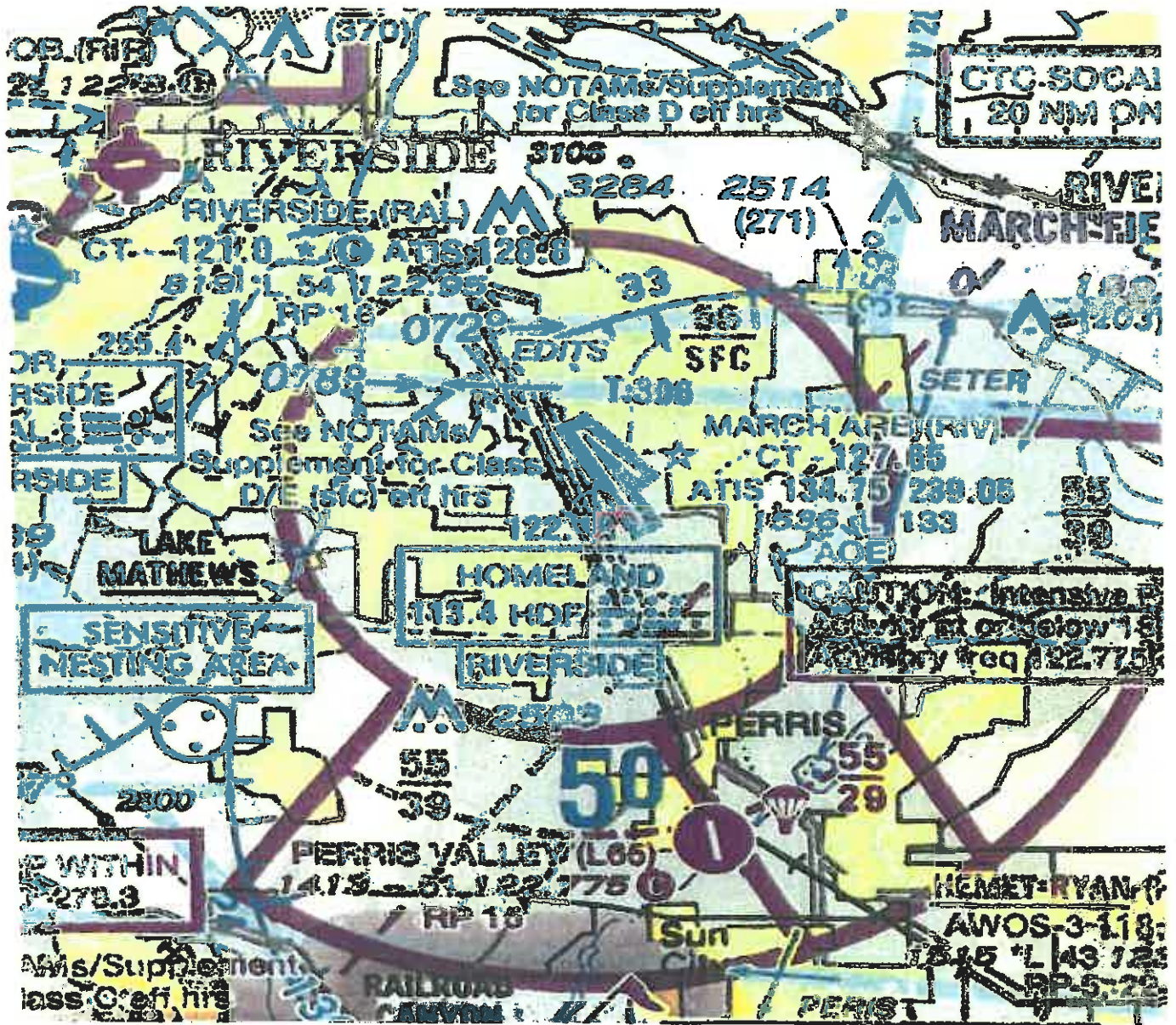
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**Signature Control No: 428280715-432020106**  
**Paul Holmquist**  
**Specialist**

( DNE )

**Attachment(s)**  
**Map(s)**







March 27, 2020

Ms. Kathy Hoffer  
Vice President  
Hillwood  
36 Discovery | Suite 120  
Irvine, California 92618

**Subject: Wildlife Hazard Review of Proposed Stormwater and Landscape Plans for the Proposed VIP 215 Project, Riverside County, California.**

**Ms. Hoffer:**

Riverside Inland Development, LLC, proposes to construct the Veterans Industrial Park 215 ("VIP 215" or "project") on 142.5 acres of property owned by the March Joint Powers Authority (JPA) at the March Inland Port in Riverside County, California. The proposed project would be constructed directly east of the I-215 off-ramp at Van Buren Boulevard, south of the existing March Field Air Museum, and west of Runway 14-32, the primary runway at March Air Reserve Base ARB (see Figure 1).

The proposed VIP 215 would operate a state-of-the-art logistics center that takes advantage of existing infrastructure that is in close proximity to the March ARB and I-215/State Route 60 to support the distribution of goods throughout the region. The project would include up to two logistics structures totaling more than 2 million square feet, loading docks, truck parking, and associated infrastructure improvements.

## **1. REGULATORY COMPLIANCE**

The proposed project requires an amendment to the Riverside County General Plan and is subject to environmental review in accordance with the California Environmental Quality Act (CEQA). As part of the CEQA analysis, an applicant must consider whether the proposed project would conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project. The proposed VIP 215 project is located within the Airport Influence Area (AIA) identified in the adopted 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan (ALUCP); therefore, the proposed project is subject to review by the Riverside County Airport Land Use Commission (ALUC) to determine its consistency with the adopted ALUCP. A determination of inconsistency by the ALUC would be considered a significant impact pursuant to CEQA.

## ALUC Review and Determination

On October 11, 2018, the Riverside ALUC considered the proposal by Riverside Inland Development, LLC. At that time the ALUC found:

- c The proposed amendment to the March JPA's General Plan land use designation is consistent with the ALUCP;
- e The proposed adoption of the VIP 215 Specific Plan is consistent with the ALUCP; and
- c The proposed construction of two industrial buildings totaling 2,185,618 square feet was consistent with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, subject to nineteen specific conditions. Three of the nineteen conditions cited by the ALUC, conditions nos. 2c, 5 and 6, were associated with the creation of potential wildlife hazards as a result of project development and serve as the focus of the following review:

2. The following uses/activities are not included in the project and shall be prohibited at the site, including:

- c. Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)

5. The proposed detention basins on the site (including water quality management basins) shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping. Trees shall be spaced to avoid creation of a continuous canopy. Landscaping in and around the detention basin shall not include vegetation that produces seeds, fruits, or berries.

6. March Air Reserve Base personnel must be transmitted for their review and approval details of the storm water conveyance system and landscaping plans.

At the request of Hillwood, Mead & Hunt, Inc., reviewed known site conditions and proposed planning documents to determine whether the proposed project would be consistent with the conditions cited by the ALUC. The review was conducted under the supervision of an FAA-qualified Airport Wildlife Biologist (QAWB) who has conducted several Wildlife Hazard Assessments and Wildlife Hazard Management Plans for Riverside County airports and is knowledgeable of the region and its ecology. Specific data considered included:

- c FAA strike records for March ARB and associated wildlife hazard management guidance documents;
- e Site-specific background studies pertaining to biological and wetland resources;
- c Proposed stormwater management plans;
- e Proposed landscaping plans and plant materials; and
- e Previous comments offered on the proposed project by the ALUC and March ARB.



## Department of the Air Force Review

The applicant submitted preliminary project plans to the Department of the Air Force in 2016. In a response letter dated September 27, 2016, the Air Force stated that they reviewed the preliminary site plans and provided eight comments, two of which were associated with aviation and wildlife.

- **Air Force Comment No. 4.** The Air Force expressed concern with rising groundwater in the Perris North sub-basin in which both March ARB and a portion of the project area reside. The Air force was concerned with the ability of the detention basins to drain within the 48 hours. The Air Force requested that underground storage be used if a 48-hour drainage time could not be achieved, as pumping is not permitted.
- **Air Force Comment No. 5.** The Air Force addressed Bird/Wildlife Aircraft Strike Hazard (BASH) concerns specific to stormwater management. The Air Force was specifically concerned with the use of existing degraded natural channels on the base property and requested that the project be connected to a larger regional stormwater effort to route stormwater around the ARB, as any new drainage onto the base would further degrade natural infrastructure, increase discharge periods, and create ponding on the airfield. Further, March ARB stated that based on the proximity to the airfield, trees that will bear mast or grow to an adequate size for roosting should not be planted.

The Air Force requested subsequent review details of the stormwater conveyance system and the landscaping plan when they became available and referred the applicant to Air Force BASH guidance.

## 2. WILDLIFE HAZARDS TO AIRCRAFT

Conflicts with aircraft and aviation have been ongoing since the start of aviation. Data compiled by the FAA indicates that the number of conflicts between wildlife and aircraft is increasing worldwide as a result of several factors, such as:

- The use of faster and quieter aircraft,
- Increased air traffic,
- Changes in land use, and
- Increased populations of many wildlife species and their adaptation to urban areas.

While most wildlife strikes do not result in extensive aircraft damage, injuries, or death, some have proven to be catastrophic and have resulted in aircraft destruction, injuries, and death. Globally, wildlife strikes have killed more than 282 people and destroyed over 283 aircraft since 1988.

FAA requires federally obligated airports to manage wildlife on their airports to promote safety and comply with the terms of their federal grant assurances and to monitor land use changes within 5 miles of the aircraft operations area (AOA). The FAA sets forth guidance for wildlife hazard monitoring and management through various advisory circulars, such as AC 150/5200-33C, *Wildlife Hazard Attractants On and Near Airports* (2020). The U.S. Air Force requires installations to establish and implement BASH programs as guided by Air Force Instruction 91-212 (2018).

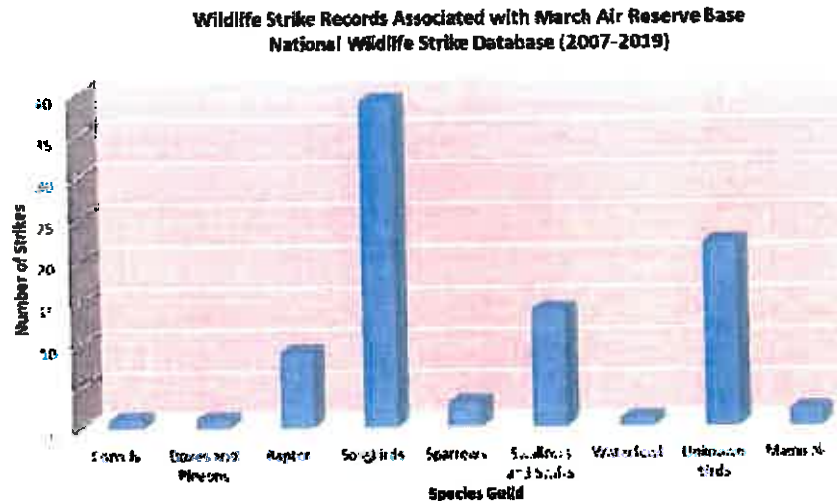
### Wildlife Strike Record for March Air Reserve Base

Since 1990, the FAA has maintained a National Wildlife Strike Database to identify the number and type of wildlife strikes that occur in the U.S. The strike data provides a scientific foundation for establishing wildlife hazard management programs and to mitigate risk. In 2018 (the most recent year for which the data have been summarized), a record 16,020 strikes were recorded in the database. Birds were involved in approximately 95 percent of the strikes, bats in approximately 3 percent, and mammals were involved 2 percent.

The FAA's National Wildlife Strike Database was reviewed to identify the number and types of wildlife strikes that have occurred with aircraft operations at March ARB. Ninety-two (92) strikes were recorded during the 13-year period from 2007 through 2019 (FAA, 2020). Although the FAA initiated the wildlife strike database in 1990, no strike reports were submitted for March ARB until 2007. Table 1 summarizes the species struck by guild. A guild represents a group of species that share common habitat or behavior.

Table 1: Wildlife Strikes at March Air Reserve Base: 2007 to 2019				
Guild or Species	Species	Scientific Name	Number of Strikes	FAA Composite Hazard Ranking
Songbirds	American pipit	<i>Anthus rubescens</i>	1	—
	American robin	<i>Turdus migratorius</i>	1	—
	Horned lark	<i>Eremophila alpestris</i>	31	15
	Perching birds spp.	<i>Passeriformes</i>	3	
	Western meadowlark	<i>Sturnella neglecta</i>	2	22
	Western tanager	<i>Piranga ludoviciana</i>	1	—
Swallows and Swifts	Cliff swallow	<i>Petrochelidon pyrrhonota</i>	11	23
	Swallows (unidentified)	<i>Hirundinidae</i>	1	23
	White-throated swift	<i>Aeronautes saxatalis</i>	2	23
Raptor	American kestrel	<i>Falco sparverius</i>	3	21
	Barn Owl	<i>Tyto alba</i>	2	14
	Ferruginous hawk	<i>Buteo regalis</i>	1	11
	Peregrine falcon	<i>Falco peregrinus</i>	1	—
	Red-tailed hawk	<i>Buteo jamaicensis</i>	2	11
Sparrows	Fox sparrow	<i>Passerella iliaca</i>	1	24
	Savannah sparrow	<i>Passerculus sandwichensis</i>	1	24
	Sparrow	<i>Passeridae</i>	1	24
Corvids	Common raven	<i>Corvus corax</i>	1	16
Doves and Pigeons	Mourning dove	<i>Zenaidura macroura</i>	1	18
Waterfowl	Northern pintail	<i>Anas acuta</i>	1	7
Unidentified birds	Not applicable		22	—
Mammals	Brazilian free-tailed bat	<i>Tadarida brasiliensis</i>	1	—
	Coyote	<i>Canis latrans</i>	1	17
<b>Total Strikes</b>			<b>92</b>	

As shown by the FAA database records and Figure 1, songbirds, swallows and swifts, and raptors were the most commonly struck birds identified. Minor aircraft damage occurred following a strike with a small unidentified bird.



More than 500 species have been identified in wildlife strike records, and the FAA has ranked 25 species groups as to their relative hazards to aircraft based on three criteria, damage, major damage, and effect on flight, and has developed a composite hazard ranking. Of the 23 species identified in association with wildlife strikes at March ARB, sixteen are ranked within the 25 most hazardous species groups by FAA.

### Site-Specific Biological Assessment

Mead & Hunt reviewed the site-specific Biological Assessment Report prepared for the proposed project, which included documentation from biological survey published data and site photographs (ELMT Consulting, 2019). The data in the report was considered with regional data and data obtained from wildlife hazard reports for nearby airports and March ARB.

Most of the project area was disturbed during the construction of March ARB and adjacent roads, drainage features, and an underground pipeline. Five drainage features are present within site boundaries, some of which include concrete linings or rip rap. Virtually no native habitat is present on site (ELMT Consulting, 2019).

On-site vegetation is composed almost entirely of non-native grassland dominated by Russian thistle with pigweed (*Amaranthus Albus*), doveweed (*Croton setiger*), jimsonweed (*Daturawrightii*), red-stemmed filaree (*Erodium cicutarium*), rattlesnake spurge (*Euphorbiaalbomarginata*), telegraph weed (*Heterotheca grandiflora*), short-podded mustard (*Hirschfeldia incana*), and horehound (*Marrubium vulgere*). The main drainage feature contains scattered stands of mulefat (*Baccharis salicifolia*), Spanish lotus (*Acmispon americanus*), common sunflower (*Helianthus annuus*), and cocklebur (*Xanthium strumarium*) throughout.

The four tributaries to the main drainage features are either primarily bare or vegetated with dense weedy plant species, primarily Russian thistle.

On-site plant communities provide foraging habitat, nesting and denning sites, and shelter from adverse weather or predation. The ELMT report states that nineteen avian species and six mammal species were identified during site field investigations. The report summarized the species observed most frequently observed as shown in Table 2.

<b>Group or Species</b>	<b>Species</b>	<b>Scientific Name</b>	<b>FAA Composite Hazard Ranking</b>
Doves and Pigeons	Rock pigeons	<i>Columba livia</i> ,	-
	Mourning dove	<i>Zenaida macroura</i>	18
Sparrows	Savannah sparrow	<i>Passerculus sandwichensis</i>	24
	White-crowned sparrow	<i>Zonotrichia leucophrys</i>	24
Songbirds	Western meadowlark	<i>Stumella neglecta</i>	22
Starlings and Blackbirds	Brewer's blackbird	<i>Euphagus cyanocephalus</i>	20
Raptors	Burrowing owl	<i>Athene cunicularia</i>	14
Mammals	Desert cottontail	<i>Sylvilagus audubonii</i>	
	San Diego black-tailed jackrabbit	<i>Lepus californicus bennettii</i>	
	California ground squirrel	<i>Otospermophilus beecheyi</i>	
	Botta's pocket gopher	<i>Thomomys bottae</i> ,	
	Coyote	<i>Canis latrans</i>	17

No nests were observed on the project site during site surveys conducted in 2015, 2018, or 2019, and few suitable nesting locations were observed on or adjacent to the project site. However, upland habitat could support local ground-nesting birds such as killdeer (*Charadrius vociferus*) and horned larks (*Eremophila alpestris*). Small pockets of mulefat growing within the main drainage provide isolated nesting opportunities. As identified previously in Table 1, horned larks were identified in more than one-third of the strikes recorded at March ARB.

### **Wildlife Summary and Conclusions**

Biological surveys were conducted in 2015, 2018 and 2019, and biologists were on site for brief periods during the three survey events. The species identified during the site-specific surveys generally coincide with those identified in the FAA Wildlife Strike Database for March ARB such as doves, sparrows, songbirds, and raptors—all of which are identified in FAA's list of the 25 most hazardous species to aircraft operations.

Additional mammal species were identified during field studies. While some of these mammals do not pose strike hazards in and of themselves, they serve as a prey base and are attractive to raptors, which are known to pose a high strike risk and have been involved in nine strikes at March ARB. Similarly, the upland

habitats were observed to provide nesting potential to horned larks, which are responsible for more than 30 strikes at March ARB.

### 3. PROPOSED STORMWATER MANAGEMENT DESIGN

Mead Hunt reviewed recent guidance from the FAA and the U.S. Air Force BASH program regarding proposed stormwater management facilities. The agency data was used to evaluate the facilities associated with the VIP 215 site.

#### Agency Guidance

The FAA discourages the development of open water facilities, including stormwater management ponds, within 10,000 feet of an air operations area (AOA) at airports serving turbine-powered aircraft. If soil conditions and other requirements allow, the FAA encourages the use of underground storm water infiltration systems because they are less attractive to wildlife.

When stormwater management systems must be located within 10,000 feet, the FAA recommends that they be designed and operated so as not to create aboveground standing water that can be attractive to various species of waterfowl. Specific recommendations include the following:

- Stormwater ponds should be designed, engineered, constructed, and maintained for a maximum 48-hour detention period after the design storm and remain completely dry between storms.
- Basins should include the use of steeply sided, rip-rap- or concrete-lined, narrow, linear-shaped water detention basins.
- When it is not possible to place these ponds away from an airport's aircraft operations area (but still on airport property), airport operators may use physical barriers, such as bird balls, wire grids, floating covers, vegetation barriers (bottom liners), or netting.

The U.S. Air Force provides guidance for the development of BASH Management Program in Airforce Instruction 91-212 dated May 31, 2018. The guidance states the following in paragraph 3.2.1.7, Wastewater Facilities, Lagoons, and Ponds:

- Installations must consider flight operations when designing and locating wastewater ponds and locate any new open water features or ponds as far from the runway and traffic patterns as possible.
- Consider pond placement to ensure transiting birds do not cross runways.
- Ponds designed with steep sides, impervious liners, little surface area, and little to no vegetation will provide reduced bird attraction.
- If pond alteration or relocation is not feasible, consider installing aeration pumps, agitation equipment, fountains, plastic bird balls/discs or grid wires (placed over the water body) to dissuade birds from utilizing holding ponds and lagoons. If spray fields are utilized, consider discharging sewage effluent during reduced flying operations.
- Consider constructing and utilizing rapid infiltration basins as a means to quickly remove water attractants where sandy soils occur.

## Design Review and Considerations

Mead & Hunt reviewed the following documents associated with the proposed project to evaluate their consistency with the guidance provided by the FAA, Air Force, and ALUC:

- The Draft *VIP 215 Specific Plan* (Specific Plan) dated January 2020;
- A site plan dated January 6, 2020, which illustrated the location of the proposed bio-retention ponds;
- A basin cross-section that was received in an email from Kathy Hoffer on February 20, 2020; and
- A memorandum from Mr. Johnny Murad, Huitt-Zollars, to Ms. Kathy Hoffer, Hillwood, dated March 23, 2020, that summarized and clarified the engineer's design and identified modifications to the site plans that had been made in response to previous recommendations from Mead & Hunt to promote consistency with FAA, Air Force, and ALUC guidance documents.

The proposed site plans identify two development scenarios that could be constructed within the project footprint: a one-building scenario and a two-building scenario. Under each scenario, the project would include the construction of bio-retention basins adjacent to the eastern project boundary and parallel to Runway 14-32. Under each scenario, the proposed bio-retention basins would cover an approximately 6.5-acre area. The Specific Plan states that stormwater would be collected by either surface flow or storm drains and directed to bio-retention/detention basins as follows:

Each basin is sized to have storage capacity for the water quality treatment volume as well as to detain and mitigate higher storm events. Water from the basins will be conveyed to an on-site overflow drain which will convey the runoff to the south and ultimately connect to a new reinforced concrete box storm drain on the south side of the project, north of Van Buren Avenue. All drainage facilities will be sized to collect and convey the 100-year storm event. All observable water in both basins will be discharged within 48 hours after the end of a storm event (Specific Plan, page 2-13).

Basin Cross-Section G-G indicates that the proposed basins would be constructed with 4:1 slopes (4 feet horizontally for every 1 foot vertically). The ponds would be equipped with curbs and gutter extending out 10 feet from either side, and each side would be fenced.

**Basin Location.** The proposed basins are located at the eastern edge of the proposed project site and adjacent to the western boundary of March ARB. The FAA recommends avoiding new open water features within 10,000 feet of aircraft movement areas, and the Air Force recommends locating new ponds as far from the runway and traffic patterns as possible.

The Applicant considered placing the proposed stormwater ponds next to the western site boundary adjacent to Interstate 215 to maximize the separation distance between the ponds and Runway 14-32 in accordance with Air Force guidance, but doing so was neither practical nor feasible due to site-specific conditions. In addition, doing so would not provide the recommended 10,000-foot separation recommended in FAA guidance.

As documented by the engineer's memo dated March 23, 2020, moving the basin system to the west side of the project site would be contrary to site topography, which slopes from northwest to southeast. Placing the basins system along the site's western boundary would require site drainage to flow against the site's natural topography. To facilitate drainage without gravity systems, the proposed project would require the use of substantially larger and deeper basins and a pump system to remove the collected water from the basin. Based on the limited capacity of the pumps and the increased size of the basins, it is unlikely that the basin system would drain within 48 hours of a storm event.

**Drainage Time.** As described in the VIP 215 Specific Plan and confirmed through the engineer's memo of March 23, 2020, the proposed pond will drain completely within 48 hours to achieve the FAA and ALUC criteria.

**Slope and Vegetation.** The FAA recommends that ponds include steep sides to prevent entry (and nesting by potentially hazardous wildlife (e.g., waterfowl). The Applicant has provided a slope of 4:1, which is the steepest allowable by County of Riverside design guidelines.

Typically, the basin bottom and side slopes are planted to promote water quality treatment and to prevent erosion. To reduce the attractiveness of the proposed basins to hazardous wildlife, the Applicant's engineer revised its design to include the use of a combination of rock and hardscape for the entire basin system pending approval by the local jurisdiction. To promote water quality, the on-site storm drain systems will be equipped with pre-treatment devices to filter out pollutants in stormwater prior to discharging the water into the basin. The use of rock scape will remove potential food sources, cover, and nesting cover for many species and make the ponds less attractive to hazardous wildlife.

**Bird Barriers.** Both the FAA and the Air Force recommend the use of physical barriers, such as netting, bird balls, or wire grids, to deter birds from open water. While the use of large grids is effective in excluding waterfowl, it is not effective in deterring smaller birds or mammals when vegetation is present. In addition, the size of the proposed basins may preclude the use of nets. Reducing basin attractiveness through the use of hardscapes would likely be more effective in discouraging wildlife from the site.

**Ongoing Maintenance.** Stormwater ponds and drains can become clogged with debris over time, leading to longer drainage times, ponding, and the growth of vegetation. In its memo dated March 23, 2020, the Applicant's engineer stated that it would identify a maintenance procedure in the project-specific Water Quality Management Plan (WQMP) for use during the life of the ponds to help ensure that the ponds continue to work properly and drain within 48 hours of a storm event. In addition to the maintenance procedure, storm water clarifiers would be installed at all storm water outlets into the basin system to ensure that clean water is deposited into the basin to help ensure that the basin bottom is not clogged with sediments and/or debris.

**Wetland Mitigation.** The proposed project will result in impacts to jurisdictional waters of the U.S. and waters of the State, and mitigation will be required by the Corps and the California Department of Fish and Wildlife. Although the Draft Specific Plan indicated that on-site mitigation efforts would be incorporated to provide compensatory mitigation, the Applicant's engineer stated in its memo of March 23, 2020, that the

on-site earthen stream would be replaced with a new Riverside County Flood Control & Water Conservation District (RCFC&WCD) storm drain system that will run along Van Buren Boulevard and around the project site. The new storm drain will likely be less attractive to hazardous wildlife than the existing earthen stream channel, which bisects the property and was observed to include isolated nesting opportunities, because it would be constructed of concrete hardscape and absent of vegetation.

#### **4. PROPOSED LANDSCAPE DESIGN**

Mead & Hunt reviewed the proposed landscape designs for the VIP 215 project to determine whether the proposed designs would be attractive to potentially hazardous wildlife observed or likely to be present in the project area. The landscape review was iterative in nature as the Applicant responded to preliminary review efforts and adjusted its plant palettes accordingly.

##### **Regulatory Background**

Plant selections, density, and the planting configurations proposed in a landscape design can influence wildlife use, abundance, and behavior. Both the FAA and Air Force identify landscaping—and especially landscaping near stormwater management facilities—as one of the greatest attractants to potentially hazardous wildlife.

FAA Advisory Circular 150/5200 33C, Section 282, offers the following recommendations to airport operators regarding landscaping and landscape maintenance:

- A QAWB should review all landscaping plans on behalf of an airport operator. Airport operators should also monitor all landscaped areas on a continuing basis for the presence of hazardous wildlife. If hazardous wildlife is detected, corrective actions should be immediately implemented to deter wildlife from utilizing these areas.
- Airport operators should ensure that plant varieties attractive to hazardous wildlife are not used on the airport. Disturbed areas or areas in need of revegetating should not be planted with seed mixtures containing millet or any other large-seed-producing grass. Plantings should follow the specific recommendations for grass management and seed and plant selection made by the State University Cooperative Extension Service, the local office of Wildlife Services, or a QAWB.
- Airport operators should also consider developing and implementing a site-specific, preferred/prohibited plant species list reviewed by a QAWB.

While the guidance cited above refers specifically to airport operators and airport facilities, the FAA's guidance is recommended to extend to the areas within 10,000 feet of aircraft movement areas as described earlier and in paragraph 1.3 of the same guidance.

The Riverside County ALUC also prepares guidance for proposed projects located in an Airport Influence Area (AIA). The guidance was developed to assist design professionals promote sustainable landscaping while minimizing hazards to aircraft operations by:

- Avoiding/preventing the creation of contiguous canopy created by trees;



- Limiting the amount of cover and massing offered by shrubs, accents, vines, and grasses to prevent the creation of habitat for birds and small mammals; and
- Preventing the natural succession of landscaping provided by groundcover by creating sharp edges between groundcover types.

The ALUC reviewed the list of California Plant Friendly Landscapes that is included in the County's *Comprehensive Landscape Guidance and Standards* and identified an abbreviated list that is appropriate for projects within the AIA. Alternative plant materials may be incorporated into project designs based on site conditions and review by a QAWB.

### **Design Review and Considerations**

Mead & Hunt reviewed Chapter 4 of the VIP 2015 Specific Plan, which provides guidelines related to landscaping and a plant materials list provided by Hillwood's consultant.

- Chapter 4.3.1, *Landscape Master Plan*, of the Specific Plan identifies the use of landscape treatments around buildings, the use of vertical trees and lower growing and broader canopy trees along Van Buren Boulevard, and a groundplane that will be "landscaped with a mix of shrubs and groundcover to create a layered appearance. The plan states that shrubs and groundcovers will be selected concurrent with final designs for individual projects in the Specific Plan area.
- Section 4.3.2, *Water Quality*, identifies the use of bioswales not only to function as stormwater/water treatment facilities but also to be integrated as a landscape feature.
- Table 4-1 of the Specific Plan provides a list of plant materials including trees, shrubs, accents, groundcover.

**Landscape Master Plan, Section 4.3.1.** Landscape guidance provided by the ALUC suggests the avoidance of continuous canopy and the use of sharp edges between types of planting. In its comments regarding the proposed project, the Air Force stated that based on the proximity to the airfield, trees that will bear mast or grow to an adequate size for roosting should not be planted. The types of trees selected and their placement should include sufficient intervals to avoid the development of mast.

The development of a layered ground plane could be contrary to this guidance that suggests the use of sharp edges between types of planting. Section 4.3.1 of the Specific Plan should be revised to reflect the guidance set forth by the ALUC for landscaping near airports.

**Water Quality, Section 4.3.2.** The discussion assumes that bio-retention basins will include earthen sides and bottoms and will be planted to further enhance water quality. As previously discussed, Mead & Hunt's recommendations pertaining to stormwater management include the use of hardscapes. While water quality enhancements maybe be achieved through the use of planting materials, the proximity to aircraft movement areas, FAA strike record, and observed wildlife species indicate that hardscapes are more appropriate at this location. In addition, The location of the proposed basins, as currently shown, is outside of public view and reduces the need to provide aesthetic enhancements.

**Plant Materials.** Mead & Hunt reviewed a portion of the proposed plant list that was provided by Hillwood in February 2019, which included accents and groundcover materials. A landscape architect reviewed the list for its potential to attract or provide habitat for hazardous wildlife. The annotated list was returned to Hillwood on February 20, 2020. Hillwood's Landscape architect, Tom Hayes of Hunter Landscape, provided a revised project plant list on March 5, 2020. The revised plant list eliminated plant materials that were not identified in ALUC landscaping guidance and proposed others for review.

Mead & Hunt reviewed the revised plant list and offers the following recommendations.

- **Trees.** Three species should be eliminated from the list because they are attractive to wildlife: Chilean mesquite (*Prosopis chilensis*), Chitalpa (*Chitalpa tashkentensis*), and Blue Palo Verde (*Cercidium* sp.). In addition, trees will not be planted on the portion of the site adjacent to the airport.
- **Shrubs:** Two species should be eliminated because they are attractive to bird species: coyote bush (*Baccharis*) and brittle brush (*Encelia farinosa*).
- **Groundcover.** Two groundcover species should be eliminated; Poverty weed (*Iva hayesiana*) and Halls honeysuckle (*Lonicera j. Halliana*), are attractive to birds. One proposed groundcover, Lantana, includes many variations, some of which produce seeds or fruit that is attractive to birds. Only non-seeding, non-fruiting selections should be used.

## 5. CONCLUSIONS AND RECOMMENDATIONS

### Project-Related Recommendations

The Riverside County ALUC found that the VIP 215 Plan was conditionally consistent with the adopted 2014 ALUCP for March ARB as long as specific conditions were achieved. Mead & Hunt reviewed the proposed one- and two-building scenario plans developed for the proposed site to determine whether they were consistent with ALUC guidance, FAA guidance, and U.S. Air Force guidance pertaining to potentially hazardous wildlife. During the review process, the Applicant provided additional clarification and incorporated several design revisions that were reflective of FAA, Air Force, and ALUC design guidance to make the proposed project site less attractive to potentially hazardous wildlife to the extent practicable.

Such measures include:

- Confirming that the proposed bio-retention/detention basin system will drain all of the collected storm water within 48 hours of a storm event.
- Providing pond slopes of 4:1, which is the maximum slope allowed by County of Riverside design guidelines.
- Proposing the use of a combination of rock and hardscape for the entire basin system rather than vegetation. This change must be approved by the local jurisdiction.
- Providing a maintenance procedure for the bio-retention/detention basin system in the project – specific Water Quality Management Plan (WQMP) to help ensure that the basins will continue to operate properly and drain within 48 hours after a storm event. In addition, storm water clarifiers will be installed at all storm water outlets into the basin system to ensure that clean water is

deposited into the basin to help ensure that the basin bottom is not clogged with sediments and/or debris.

- o Replacing the existing earthen stream that bisects the property with a new Riverside County Flood Control & Water Conservation District (RCFC&WCD) storm drain system that will run along Van Buren Boulevard and around the subject site.
- o Eliminating trees from site landscaping plans; and
- o Revising the plant palette presented in the Specific Plan to include species that would not be attractive to hazardous wildlife.

The incorporation of site-specific modifications and recommendations for subsequent site development identified in this letter report combined with the submission of revised plans for Air Force review and concurrence will promote consistency with ALUC condition nos. 2c, 5, and 6, and ALUC design guidance.

### **Subsequent Site Development**

Future site development plans for the VIP 2015 site will need to be consistent with the 2014 ALUCP for the March ARB and the attached ALUC design guidance. Mead & Hunt recommends that the ALUC design guidance for landscaping and stormwater, FAA AC 150/5200-33C, *Wildlife Hazard Attractions On and Near Airports*, and Air Force Instruction 91-21231, *Birds/Wildlife Aircraft Strike Hazard (BASH) Management Program*, be considered in subsequent site development and identified in the Specific Plan.

In addition, Mead & Hunt recommends that Section 4 of the VIP 215 Specific Plan be revised as follows to promote consistency with the 2014 ALUCP and ALUCP design guidance:

- o Section 4.3.1 should be revised to reflect the goals of the ALUC for landscaping within the AIA and set forth in its guidance "Landscaping Near Airports." The section should include a revised version of Table 4-1 that reflects the memo from Hunter Landscaping dated March 5, 2020, and the recommendations cited above for trees, shrubs, and groundcover.
- o Section 4.3.1 should be revised to state that subsequent landscape plans created by tenants for portions of the VIP site must adhere to the Specific Plan and plant materials identified and guidance set forth by the ALUC and the Applicant's goal of using only plant materials that are acceptable following review by a QAWB. This language should be included in development agreements as well.
- o Section 4.3.2 should be revised to reflect the use of hardscape for proposed stormwater management basins.

The Riverside County ALUC found that the VIP 215 Plan was conditionally consistent with the adopted 2014 ALUCP for March ARB as long as specific conditions were achieved. The recommendations made by Mead & Hunt and subsequent design revisions made by the Applicant are intended to discourage and reduce the site's attractiveness to potentially hazardous wildlife. The design modifications described in this letter report and the subsequent submission of revised plans for Air Force review and concurrence will promote consistency with ALUC condition nos. 2c, 5, and 6.

Kathy Hoffer  
March 27, 2020  
Page - 14

Thank you for this opportunity to review the site plans and planning documents for the VIP 2015 development. Should you have any questions, please reach out to me or Lisa Harmon.

Sincerely,  
MEAD & HUNT, INC.



Rick Jones, FAA-Qualified Airport Wildlife Biologist

**Attachments:**

Riverside County ALUC Stormwater Management Guidance: Airports, Wildlife and Stormwater Management  
Riverside County ALUC Landscaping Management Guidance: Landscaping Near Airports

## **Attachments**

**Riverside County ALUC  
Stormwater Management Guidance:  
Airports, Wildlife and Stormwater Management**

**Low-Impact Development.** In recent years, Riverside County has focused on Low-Impact Development (LID), which includes techniques to filter, store and retain runoff on-site. LID BMPs retain runoff to optimize infiltration/recharge, and many promote the use of vegetation to provide for the uptake of pollutants. Although LID BMPs can provide environmental, economic and community benefits, they can retain open water for prolonged periods and attract hazardous wildlife. Many LID BMPs are incompatible with aircraft operations and must be considered with caution within the AIA.

**Aviation-Specific Stormwater Management.** FAA acknowledges that project-related BMPs must consider many non-aviation factors, such as soil types, space requirements, maintenance, constructability, etc. United States Department of Agriculture (USDA) and FAA have identified specific design characteristics that should be considered during BMP design and incorporated to make most BMPs less attractive to wildlife (Table 2).

**ADAPTIVE MEASURES**

When open water detention ponds must be used within the AIA, the ponds may be equipped with bird balls, floating covers, nets, or overhead wires to cover open water and discourage use by hazardous wildlife. For example, concrete basins are unlikely to attract wildlife, and pond liners can prevent the development of hydrophytic vegetation. These technologies must be used with caution and only in areas with controlled access.



Infiltration trenches detain water for brief periods. This trench at Seattle-Tacoma Airport includes vegetation appropriate for an airport environment.



Bioretention facilities can provide food and shelter for potentially hazardous wildlife, but may be suitable with modification.

BMP	Compatibility within the AIA
Infiltration Trenches Recommended	<ul style="list-style-type: none"> <li>• Suitable because water infiltrates below ground surface.</li> <li>• Vegetation must be selected and approved by a FAA-qualified Airport Wildlife Hazard Biologist (qualified biologist) to discourage wildlife.</li> </ul>
Permeable Pavement Recommended	Desirable because water does not pool on parking lots and other paved surfaces that are not high-traffic areas.
Harvest and Use (H&U) Not recommended	Suitable as long as water is stored in a sealed tank.
Sand Filter Basins Recommended	Desirable because they promote water to flow through an underdrain system.
Vegetated Filter Strips and Vegetated Swales Recommended	Desirable because neither BMP involves retained water. However, vegetation must be selected to discourage hazardous wildlife and reviewed by a qualified biologist.
Water Quality Inlets Recommended	Desirable because they do not contain ponded water. Associated vegetation must be selected to discourage hazardous wildlife and reviewed by a qualified biologist.

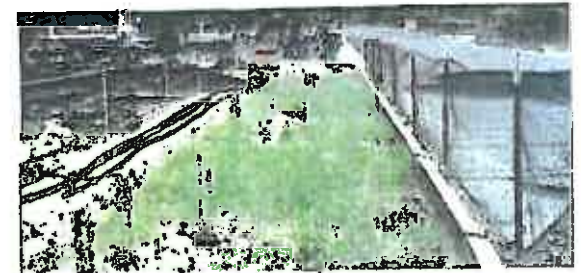
**Infiltration Basins**  
Not recommended without Modification  
Suitable only if design addresses wildlife hazards

- Unsuitable in AUCFP Compatibility Zone A.
- Suitable in Zones B and C with appropriate modifications, such as: Drawdown within 48 hours or manufactured cover to prevent view and availability of open water; and absence of landscape or landscaping approved by a qualified biologist.
- Steep slopes (steeper than 3:1).

**Bioretention Facilities**  
Not Recommended without Modification (also known as rain gardens, bioretention basins, infiltration basins, landscaped filter basins)

Although bioretention can mask open water, BMP is not recommended for airports based on its potential to provide food, water, and shelter for hazardous wildlife.

- Unsuitable in Compatibility Zone A.
- Potentially suitable in Zones B and C only when small in size (e.g., parking islands, site entrances, planter boxes, etc.) and when vegetation is selected to discourage hazardous wildlife and reviewed by a qualified biologist.
- Potentially suitable in Zones D and E when basin is less than 30 feet in length/width; and vegetation is selected to discourage hazardous wildlife and reviewed by a qualified biologist.



Small bioretention facilities that provide sparse vegetation may be suitable in an aviation environment.



Extended detention basins are frequently used to serve both water quality management and to provide amenities. These basins hold water and would not be appropriate within an AIA because of the open water.



Sand filter at the base of the bioswale promotes infiltration.



Porous pavements allow water to infiltrate to a soil layer below the surface.



Adaptive measures such as liners, a concrete basin, and overhead wire grid can make extended detention strategies less attractive to hazardous wildlife.



Vegetated bioswales improve water quality and prevent water accumulation. However, dense and tall vegetation may be attractive to hazardous wildlife.



Infiltration basins with rock bottoms are less attractive to birds because they mask water and do not provide vegetation.



### STORMWATER BEST MANAGEMENT PRACTICES

Riverside County and its incorporated cities require water quality/stormwater management controls for development and redevelopment projects. The Riverside Conservation District has prepared a separate Water Quality Management Plan for each watershed in the County that identifies treatment control Best Management Practices (BMPs) for improving water quality and managing stormwater volumes/flows following the design storm (i.e., 24-hour storm). Structural BMPs identified in Riverside County guidance and their compatibility within the AIA are summarized in Table 1.

### ADDITIONAL RESOURCES/MORE INFORMATION:

- Riverside County Flood Control and Water Conservation District, Water Quality Management Webpage. Available at: <http://www.floodcontrol.org/>
- FAA Advisory Circular 150/5200-33, "Wildlife Hazard Attractants On and Near Airports": [http://www.faa.gov/air\\_traffic/operations/resources/acf/wildlife\\_hazard\\_attracants\\_on\\_and\\_near\\_airports/](http://www.faa.gov/air_traffic/operations/resources/acf/wildlife_hazard_attracants_on_and_near_airports/)
- Airport Cooperative Research Program, Balancing Airport Stormwater and Bird Hazard Management: [http://www.fhwa.dot.gov/airports/airport\\_cooperative\\_research\\_program/ACRP\\_001/ACRP\\_001\\_report.pdf](http://www.fhwa.dot.gov/airports/airport_cooperative_research_program/ACRP_001/ACRP_001_report.pdf)

Table 2. Recommended Measures to Reduce Wildlife Attraction Associated with Stormwater BMPs

BMP Characteristic	Recommended Design Measure
<b>Exposed Surface Water</b> <ul style="list-style-type: none"> <li>• Especially attractive to waterfowl, shorebirds, and flocking birds.</li> <li>• Provides source for drinking and nest building.</li> <li>• More attractive when constructed near other open water features or ponds.</li> </ul>	<ul style="list-style-type: none"> <li>• Reduce availability by providing 48-hour drawdown following a design storm (i.e., 24-hour storm).</li> <li>• Cover using bird balls.</li> <li>• Consider earth-bottom culverts, French drains, trench covers, and underground storage options.</li> <li>• Avoid within 8 km (5 miles) of other open water features or facilities.</li> </ul>
<b>Vegetation and Landscaping</b> <ul style="list-style-type: none"> <li>• Provides food.</li> <li>• Tall vegetation provides shelter and nesting opportunities.</li> <li>• Diverse vegetation attracts more diverse wildlife.</li> </ul>	<ul style="list-style-type: none"> <li>• Eliminate vegetation (concrete banks, steep slopes, etc.).</li> <li>• If necessary, provide a monoculture or decreased diversity.</li> <li>• Never use species that provide a food source (seeds, berries, nuts, and drupes).</li> <li>• Provide regular maintenance to prevent seeding and shelter.</li> </ul>
<b>Aspect/Geometry</b> <ul style="list-style-type: none"> <li>• Slopes can provide opportunities for nesting and loafing.</li> </ul>	<b>Avoid or reduce available shoreline:</b> <ul style="list-style-type: none"> <li>• Implement narrow, linear trenches rather than open water or regular circles as pond shapes.</li> <li>• Create steep slopes (&lt;3:1).</li> <li>• Avoid irregular shapes for basins.</li> <li>• Avoid vegetation.</li> </ul>

### WHAT YOU CAN DO:

Airport operators, developers and communities must work together to manage stormwater in the airport vicinity to reduce hazards to air travelers and the public while addressing site-specific challenges.

- Identify whether your project is near an airport and in an AIA or critical area. (<http://www.rcaluc.org/Portals/0/PDFGeneral/Plan/Wildlife%20Attraction%20-%20FAA%20Review.pdf>)
- Work with the airport operator, ALUC, and city/county staff to identify an acceptable water quality management strategy.
- Contact the applicable airport to review your stormwater plans or request plan review by a FAA-qualified wildlife biologist. The form is available at: <http://www.rcaluc.org/Portals/0/PDFGeneral/Form/Wildlife%20Attraction%20-%20FAA%20Review.pdf>.



## AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT

### GUIDANCE FOR PROPOSED PROJECTS IN AN AIRPORT INFLUENCE AREA

Riverside County includes diverse topography and is home to three watersheds and a portion of the Salton Sea, an important stop along the Pacific Flyway for migrating bird species. The County's arid climate makes water quality management and water conservation paramount.

The County is also the home to Palm Springs International Airport, 12 public use general aviation airports, and the March Air Reserve Base, whose operations can be challenged by the presence of hazardous wildlife such as raptors, water-fowl, doves/pigeons, gulls, fowl birds, and mammals (coyote and deer). Since 1990, more than 100 wildlife strikes with aircraft have occurred in Riverside County, some of which have led to substantial aircraft damage. Most strikes occur at low altitude (less than 3,500 feet above runway height). Much of the geographic area associated with these altitudes coincides with an Airport Influence Area (AIA) as defined in the Riverside County Airport Land Use Compatibility Plan (ALUCP).

### AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT

The Federal Aviation Administration (FAA) identifies stormwater management facilities on and near airports as one of the greatest attractants to hazardous wildlife. Many species are attracted to open water features and associated vegetation that offers water, food, and shelter. The FAA warns against the construction of new open water bodies or mitigation sites within 10,000 feet of aircraft movement areas and within 5 miles of approach/departure surfaces (FAA Advisory Circular 150/5200-33B).



Remains of an owl ingested by an aircraft engine.

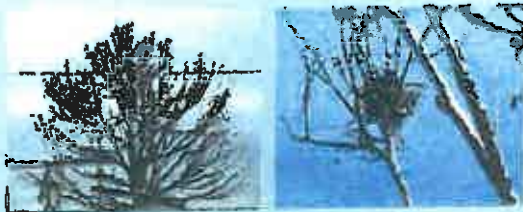




**Riverside County ALUC  
Landscaping Management Guidance:  
Landscaping Near Airports**



**Acceptable**  
The trees above have a vertical branching structure that minimizes perching and nesting opportunities



**Not acceptable.**  
Examples of trees that are attractive to birds because of horizontal branching structure.



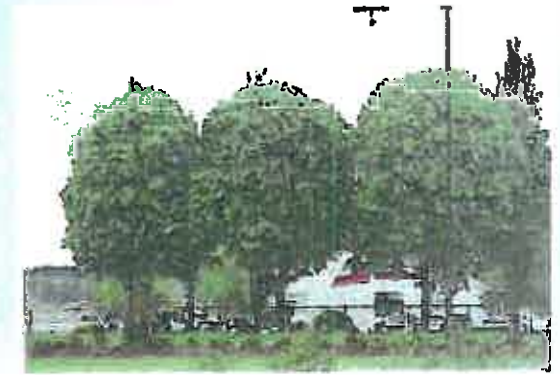
**Not acceptable**  
Trees, shrubs and plants that produce wildlife edible fruit and seeds should be avoided



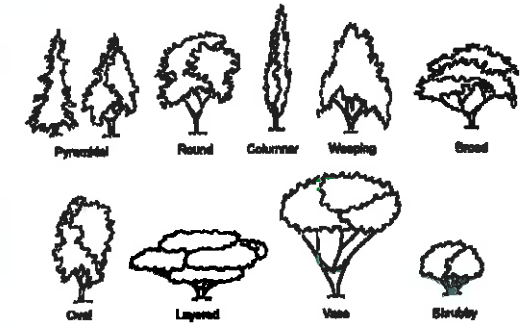
Landscaping needs to be aesthetically pleasing, but it must coincide with the responsibility for aviation safety.

**TABLE 2. Acceptable Plants from Riverside County Landscaping Guide**

Plant Family	Common Name	AVIATION HEIGHT	AVIATION WIDTH	
TREES	<i>Cercis occidentalis</i>	Western Redbud	VL: 1, 2; L: 3-4	2-24
	<i>Olea europaea</i> 'Swan Hill'	Fruitless Olive	GL: 1, 2; L: 3, 4; M: 5, 6	4-9; 11-24
	<i>Pinus</i> spp.	Pine, various species	Varies by species	Varies by species
	<i>Rhus amara</i>	African Sumac	L: 1-4; M: 5-6	8-9; 12-24
	<i>Robinia neomexicana</i> *	Desert Locust	L: 1-4; M: 5-6	2-3, 7-11, 14, 18-24
	<i>Robinia x ambigua</i>	Locust	L: 1-4; M: 5-6	2-24
	<i>Ulmus parvifolia</i>	Chinese Elm	M: 1-6	2-24
	<i>Aloysia triphylla</i>	Lemon Verbena	L: 1-6	9-10; 12-21
	<i>Cobus</i> spp.	Rockrose	L: 1-6	4-8, 14-24
	<i>Dalea parda</i>	Bush Dalea	L: 6	12, 17
SHRUBS	<i>Encelia farinosa</i>	Brittlebush	VL: 1; L: 3-6	
	<i>Grevillea Noellii</i>	Noel's Grevillea	L: 1-4; M: 6	
	<i>Aspidistra californica</i>	Chuparosa	M: 1, 4; VL: 3; L: 4-5	
	<i>Leucaena canariensis</i>	Bush Lantana	L: 1-4; M: 6	
	<i>Lavandula</i> spp.	Lavender	L: 10; M: 5-6	2-24; varies
	<i>Handia domestica</i> species	Heavenly Bamboo	L: 1-4; M: 5-6	
	<i>Ruscus aculeatus</i> 'Tuscan Blue'	Tuscan Blue Rosemary	L: 1-4; M: 5-6	
	<i>Salvia greggii</i>	Autumn sage	L: 1-4; M: 5-6	
	<i>Artemisia pyramidalis</i>	Sandhill Sage	VL: 1	
	<i>Oenothera caespitosa</i>	White Evening Primrose	L: 1-2, 3-5	10, 7-14, 18-21
GROUND COVER	<i>Oenothera stibbei</i>	Baja Evening Primrose	L: 1-6	10-13
	<i>Penstemon brecherifolius</i>	Del Rio	L: 4-6	10-13
	<i>Psychotrochium jamaicense</i>	Star Jasmine	M: 1-6	8024
	<i>Zauschneria californica</i>	California Fuchsia	L: 1, 2, 4; VL: 3; M: 5-6	2011, 14-24
	<i>Cortaderia distachya</i> (Syn. <i>C. selowiana</i> )	Pampass Grass	N/A	N/A
	<i>Festuca</i> spp.	Fescue	Varies by Species	Varies by Species
	<i>Zoysia 'Victoria'</i>	Zoysia Grass	60% of ETO	8-9, 12-24
	<i>Agave</i> species	Agave	L: 1-4, 6	10, 12-24 (Notes)
	<i>Aloe</i> species	Aloe	L: 1-4, 6	8-4, 12-24
	<i>Chondropetalum fraxetorum</i>	Cape Rush	M: 1; M: 3	8-9, 12-24
GRASSES	<i>Dasythrix</i> species	Desert Spoon	VL: 1, 4-6	10-24
	<i>Deschampsia cespitosa</i>	Tufted Hair Grass	L: 1-4	2-24
	<i>Festuca (ovina) glauca</i>	Blue Fescue	L: 1-3; M: 3-6	1-24
	<i>Diates bicolor</i>	Fortnight Lily		VL: 1; L: 3-6
	<i>Echinocactus grusonii</i>	Golden Barrel Cactus	VL: 1-2; L: 3-4, 6	12-24
	<i>Fouquieria splendens</i>	Octillo	L: 1, 4-6; VL: 3	10-13, 18-20
	<i>Hesperaloe parviflora</i>	Red / Yellow Yucca	VL: 3; L: 4-6	24, 3, 7-16, 18-24
	<i>Muhlenbergia rigens</i>	Deer Grass	L: 1, 3; M: 2, 4-6	4-24
	<i>Opuntia</i> species	Prickly Pear, Cholla	VL: 1-3; L: 4-6	Varies by Species
	<i>Penstemon parryi</i>	Parry's Beardtongue	L: 1-6	10-13
JACQUEE GRASSES	<i>Penstemon superbus</i>	Superb Beardtongue	L: 1-6	10-13
	<i>Tillandsia violacea</i>	Society Garlic	M: 1-4, 6	13-24
	<i>Yucca</i> species	Yucca	L: 1-6	Varies by Species



Not recommended are trees that overlap, allowing birds to move safely from tree to tree without exposure to the weather or predators.



Trees approved for planting should have varied canopy types and varied heights, both at time of planting and at maturity. A combination of the styles illustrated above is recommended.

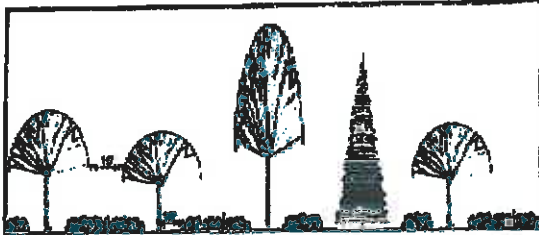


Figure 1. Selection of shrubs should be a mix of deciduous and coniferous species with no more than 30 percent evergreen species.

**Plant Selection, Irrigation, and Wildlife Management.** Riverside County requires landscaping for proposed development and redevelopment projects, and it is also committed to the use of native and drought-tolerant plants to reduce landscape-related water use. The County of Riverside Guide to California Friendly provides a lengthy plant palette to help landscape architects, planners, and the public select plant materials that will reduce water use in accordance with local and state goals. Use of native and drought-tolerant plants is encouraged. [California Friendly Landscaping Tool](#)

Many of the plants on the "County of Riverside California Friendly Plant List" could attract potentially hazardous wildlife species. Table 2 provides a reduced species list, nearly all of which were excerpted from the Friendly Plant List, but are less likely to support potentially hazardous wildlife. Project sponsors should use this list for projects within an AIA.

The list is not meant to be exhaustive, and other species may be appropriate based on the project location or other project-related circumstances. Sponsors who wish to propose plant materials that are not included in Table 1 will need to demonstrate to the ALUC that proposed species will be unlikely to attract hazardous wildlife to the AIA.

**General Guidelines.** Other factors can affect wildlife behavior. Landscaping can provide a food source, opportunities for shelter, nesting and perching. Proposed landscaping can help to discourage wildlife through the application of the following guidelines summarized below and described in Table 1.

- **Close the Restaurant!** Do not use plant material that produce a food source, such as edible fruit, seeds, berries, drupes, or palatable forage for grazing wildlife. When possible, select a non-fruiting variety or male cultivar.
- **No Vacancy!** Avoid densely branched or foliated trees; they provide ideal nesting habitat and shelter.
- **Prevent Littering!** Select tree species that exhibit a vertical branching structure to minimize nesting and perching opportunities (Figure 1).

Table 1. Design Guidance for Plant Materials

TREES	<p><b>Avoid/Prevent Contiguous Canopy</b></p> <ol style="list-style-type: none"> <li>1. Prevent overlapping crown structures. Contiguous crowns can provide safe passage for wildlife. Provide sufficient distance between plants to ensure that at least 15 feet of open space will remain between mature crowns (Figure 1).</li> <li>2. Prevent homogenous canopy types and tree height. Variable canopy height will reduce thermal cover and protection from predators. <ul style="list-style-type: none"> <li>■ Provide significant variation between the type of canopy and height of the species, both at planting and at maturity.</li> <li>■ Provide no more than 20% evergreen species on site, and never plant evergreens in mass or adjacent to each other.</li> </ul> </li> </ol>
SHRUBS/ACCENTS/GRASSES	<p><b>Limit Coverage</b></p> <p>Limit the amount of cover and avoid massing to prevent the creation of habitat for birds or small mammals.</p> <ul style="list-style-type: none"> <li>■ Mix deciduous, herbaceous, and evergreen species.</li> <li>■ Do not plant species in mass. At a minimum, provide sufficient spacing to equal the width of each species at maturity. Avoid species with the potential to creep near shrubs (Figure 2).</li> <li>■ Provide at least 10 feet between trees and other species greater than 1 foot in height.</li> </ul>
GROUNDCOVER/TURF	<p><b>Prevent the natural succession of landscape!</b></p> <p>Groundcover plays a transitional role between shrubs, grasses, and trees, and this succession creates an ideal habitat for diverse wildlife (see Figure 2).</p> <ol style="list-style-type: none"> <li>1. Provide a buffer and sharp edges between groundcover, turf, shrubs and trees, using hardscape or mulching.</li> <li>2. When possible, use alternative groundcovers, such as decorative paving and hardscapes instead of planted groundcover/turf.</li> <li>3. The use of groundcover/turf may be impractical or undesirable based on irrigation needs or site-specific conditions. Consider using the following: <ul style="list-style-type: none"> <li>■ Artificial turf in place of groundcover, which can reduce maintenance and eliminate irrigation needs (Figure 2A).</li> <li>■ Porous concrete to cover smaller areas (Figure 2B).</li> <li>■ Permeable pavers to provide visual interest while promoting drainage (Figure 2C).</li> </ul> </li> </ol>
VINES	<p><b>Limit Coverage</b></p> <p>Limit the amount of cover and avoid massing to prevent the creation of habitat for birds or small mammals.</p> <ul style="list-style-type: none"> <li>■ Do not use vines to create overhead canopy or to cover structures.</li> <li>■ Do not plant vines to grow on the trunk or branches of trees.</li> <li>■ Minimize vines to areas of 5 feet or less in width. Vines require considerably more maintenance than other plant materials.</li> </ul>

Acceptable plants from the Riverside County Landscaping Guide



## LANDSCAPING NEAR AIRPORTS: Special Considerations for Preventing or Reducing Wildlife Hazards to Aircraft

Landscaping makes a visual statement that helps to define a sense of space by complementing architectural designs and contributing to an attractive, inviting facility. In some cases, a landscaping plan can be used to restore previously disturbed areas. However, such landscape plans are not always appropriate near airports.

Wildlife can pose hazards to aircraft operations, and more than 150 wildlife strikes have been recorded at Riverside County. The Riverside County Airport Land Use Commission (ALUC) prepared this guidance for the preparation of landscape designs to support FAA's efforts to reduce wildlife hazards to aircraft. This guidance should be considered for projects within the Airport Influence Area (AIA) for Riverside County Airports. The following landscape guidance was developed by planners, landscape architects and biologists to help design professionals, airport staff, and other County departments and agencies promote sustainable landscaping while minimizing wildlife hazards at Riverside County's public-use airports.

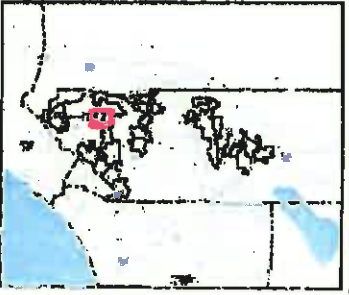
**Discouraging Hazardous Wildlife.** Plant selections, density, and the configuration of proposed landscaping can influence wildlife use and behavior. Landscaping that provides a food source, perching habitat, nesting opportunities, or shelter can attract raptors, flocking birds, mammals and their prey, resulting in subsequent risks to aviators and the traveling public.



Figure 2 - Alternative landscaping and maintenance turf

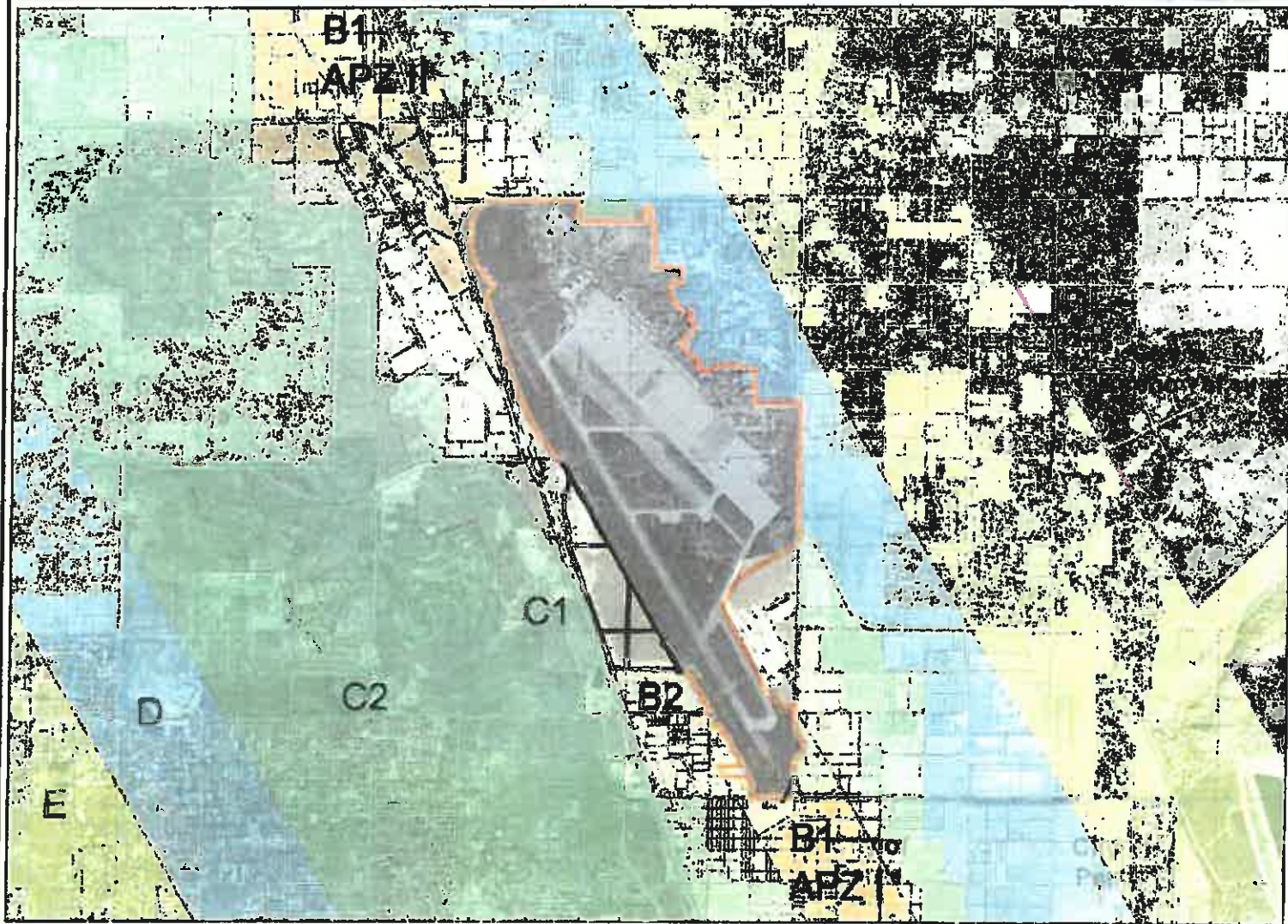


# My Map



**Legend**

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC8
- C2-HIGHT



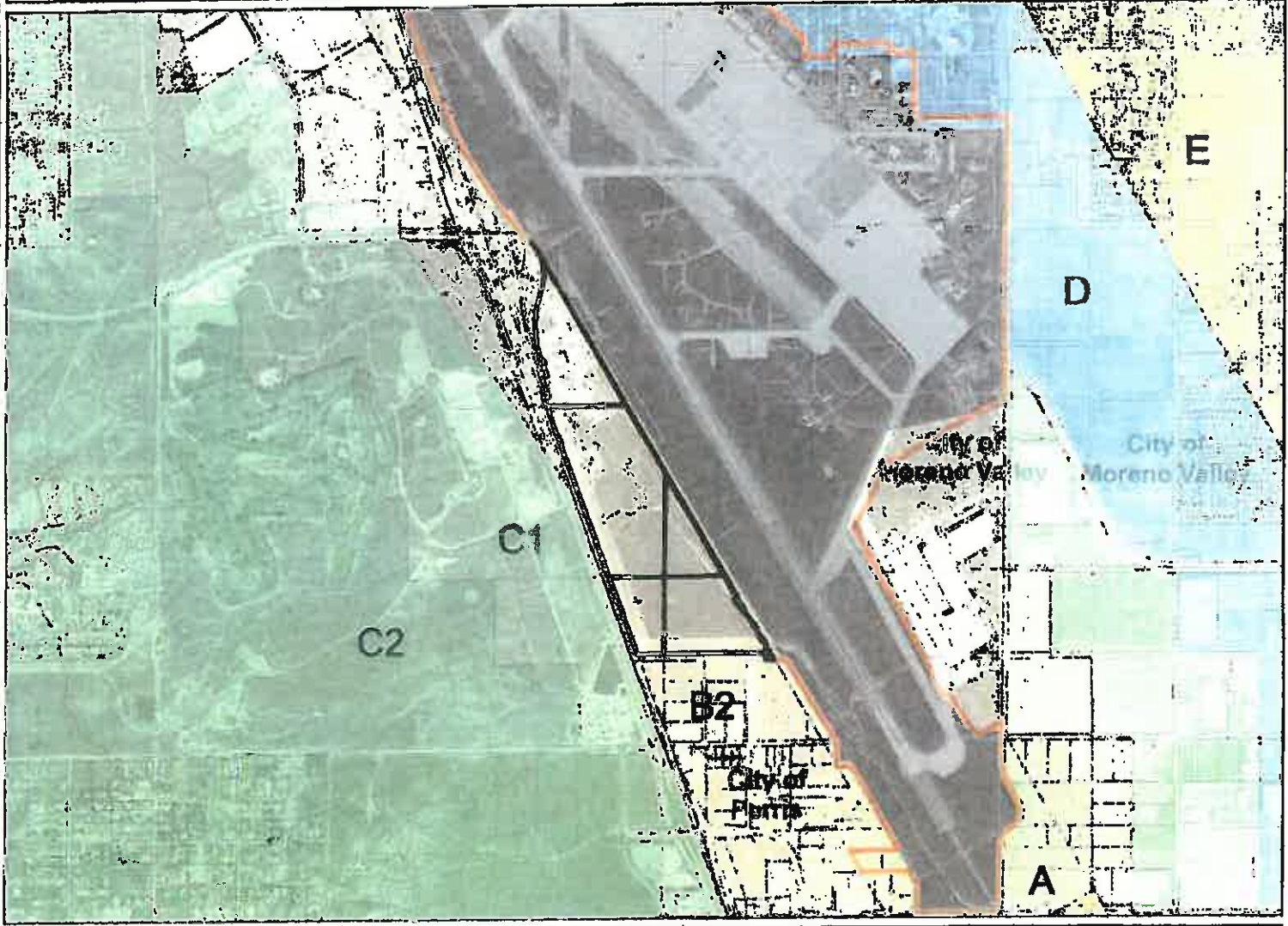
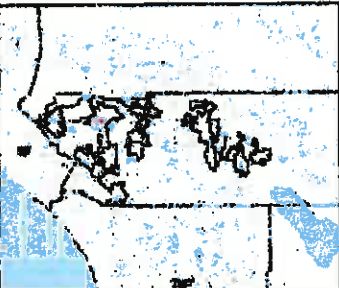
**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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**Notes**

# My Map

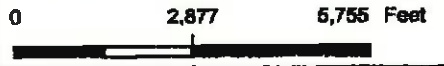


**Legend**

Airports  
 AIA  
 Airport Compatibility

**OTHER ZONE**

- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
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- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC8
- C2-HIGHT

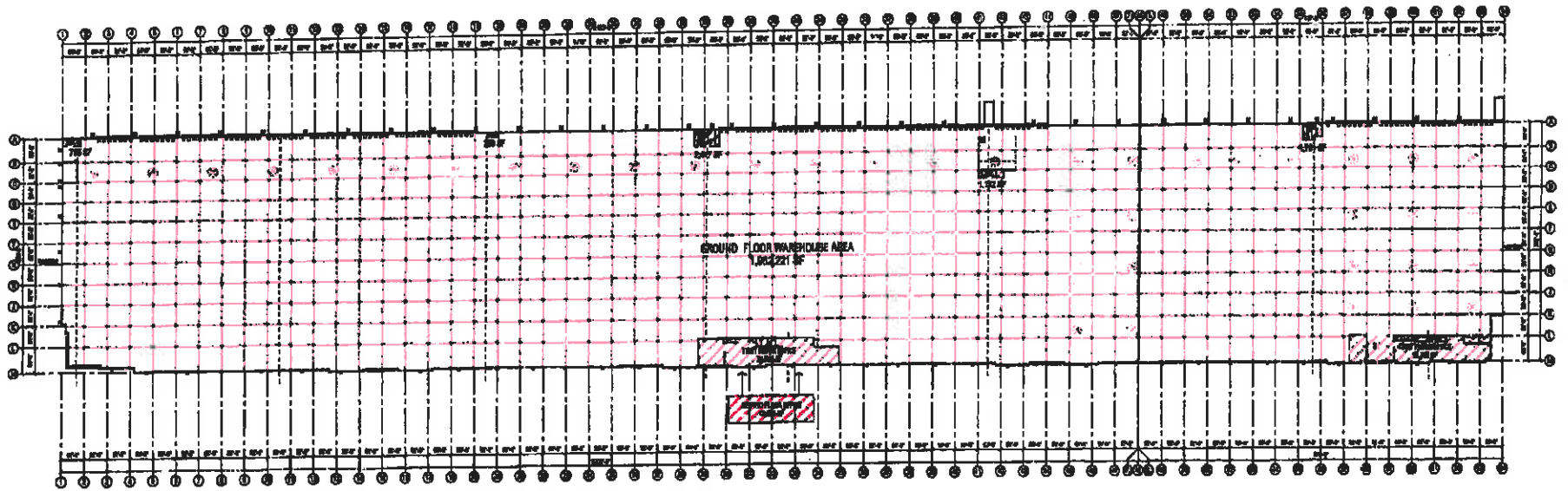


**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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**Notes**



TOTAL GROUND FLOOR OFFICE AREA: 46,637 SF  
 TOTAL GROUND FLOOR WAREHOUSE AREA: 1,962,221 SF  
 TOTAL SECOND FLOOR OFFICE: 13,506 SF  
 TOTAL BUILDING AREA: 2,022,364 SF

BUILDING PLAN





Revision table with columns for No., Description, and Date.

VETERANS INDUSTRIAL  
PARK 216  
M.J.P.A.  
COUNTY OF RIVERSIDE, CA

HILLWOOD  
RIVERSIDE ISLAND  
DEVELOPMENT, LLC  
HW 104 INDUSTRIAL DRIVE 104  
CORONA, CA 92618  
951-244-7770 FAX

Table with columns for Date, No., Description, and Date.

GENERAL LAYOUT PLAN

### PROJECT DATA

PROJECT NO. 04-111	DATE OF THIS PLAN 10/15/04
CITY OF RIVERSIDE	PLANNING DEPARTMENT
PROJECT NAME	GENERAL INVESTIGATION
CITY ENGINEER	DATE OF REVIEW
CITY PLANNING DIRECTOR	DATE OF REVIEW
CITY MANAGER	DATE OF REVIEW
COMMISSIONER	DATE OF REVIEW
COMMISSIONER	DATE OF REVIEW
COMMISSIONER	DATE OF REVIEW
COMMISSIONER	DATE OF REVIEW

### UTILITY PROVIDERS

AT&T	AT&T
CALIFORNIA GAS	CALIFORNIA GAS
CHRYSLER FINANCIAL	CHRYSLER FINANCIAL
CHRYSLER FINANCIAL	CHRYSLER FINANCIAL
CHRYSLER FINANCIAL	CHRYSLER FINANCIAL

### ZONING INFORMATION

GENERAL INFORMATION ON ZONING AND PLANNING DEPARTMENT'S ZONING ORDINANCES. THIS SECTION IS FOR INFORMATION ONLY AND IS NOT A SUBSTITUTE FOR THE ZONING ORDINANCES.

### APPLICABLE ZONING

ZONING DISTRICT: I-216  
ZONING SUBDISTRICT: 104  
ZONING MAP: 104  
ZONING DESCRIPTION: INDUSTRIAL PARK

### LEGAL DESCRIPTION

THE LEGAL DESCRIPTION OF THIS PROJECT IS AS SHOWN ON THE ZONING MAP. THIS PROJECT IS LOCATED IN THE ZONING DISTRICT OF I-216, ZONING SUBDISTRICT 104, ZONING MAP 104.

### GENERAL INFORMATION

GENERAL INFORMATION ON THE PROJECT, INCLUDING THE PROJECT NAME, ADDRESS, AND CONTACT INFORMATION. THIS SECTION IS FOR INFORMATION ONLY AND IS NOT A SUBSTITUTE FOR THE ZONING ORDINANCES.

### GENERAL NOTES

1. THE PROJECT IS LOCATED IN THE ZONING DISTRICT OF I-216, ZONING SUBDISTRICT 104, ZONING MAP 104.

2. THE PROJECT IS SUBJECT TO THE ZONING ORDINANCES OF THE CITY OF RIVERSIDE.

3. THE PROJECT IS SUBJECT TO THE PLANNING DEPARTMENT'S ZONING ORDINANCES.

4. THE PROJECT IS SUBJECT TO THE CITY OF RIVERSIDE'S ZONING ORDINANCES.

### STANDARD PLAN NOTES

1. THE PROJECT IS LOCATED IN THE ZONING DISTRICT OF I-216, ZONING SUBDISTRICT 104, ZONING MAP 104.

2. THE PROJECT IS SUBJECT TO THE ZONING ORDINANCES OF THE CITY OF RIVERSIDE.

3. THE PROJECT IS SUBJECT TO THE PLANNING DEPARTMENT'S ZONING ORDINANCES.

4. THE PROJECT IS SUBJECT TO THE CITY OF RIVERSIDE'S ZONING ORDINANCES.

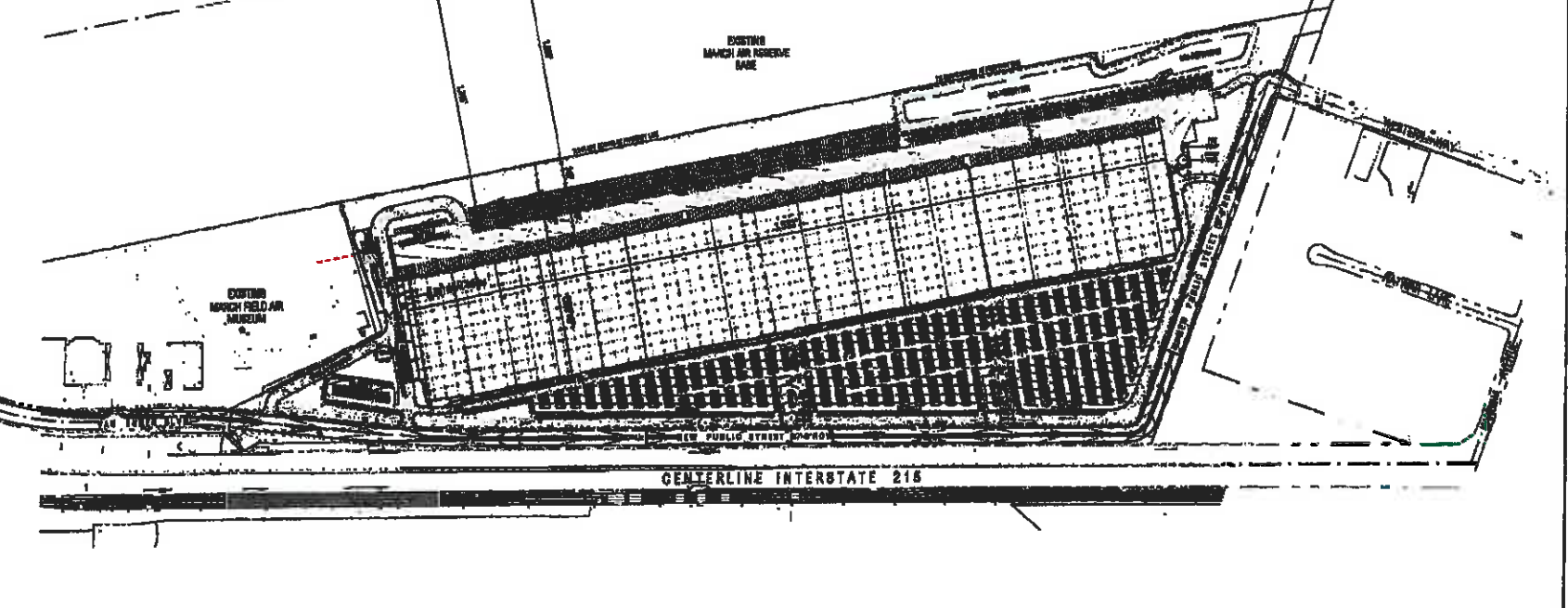
### STANDARD PLAN NOTES

1. THE PROJECT IS LOCATED IN THE ZONING DISTRICT OF I-216, ZONING SUBDISTRICT 104, ZONING MAP 104.

2. THE PROJECT IS SUBJECT TO THE ZONING ORDINANCES OF THE CITY OF RIVERSIDE.

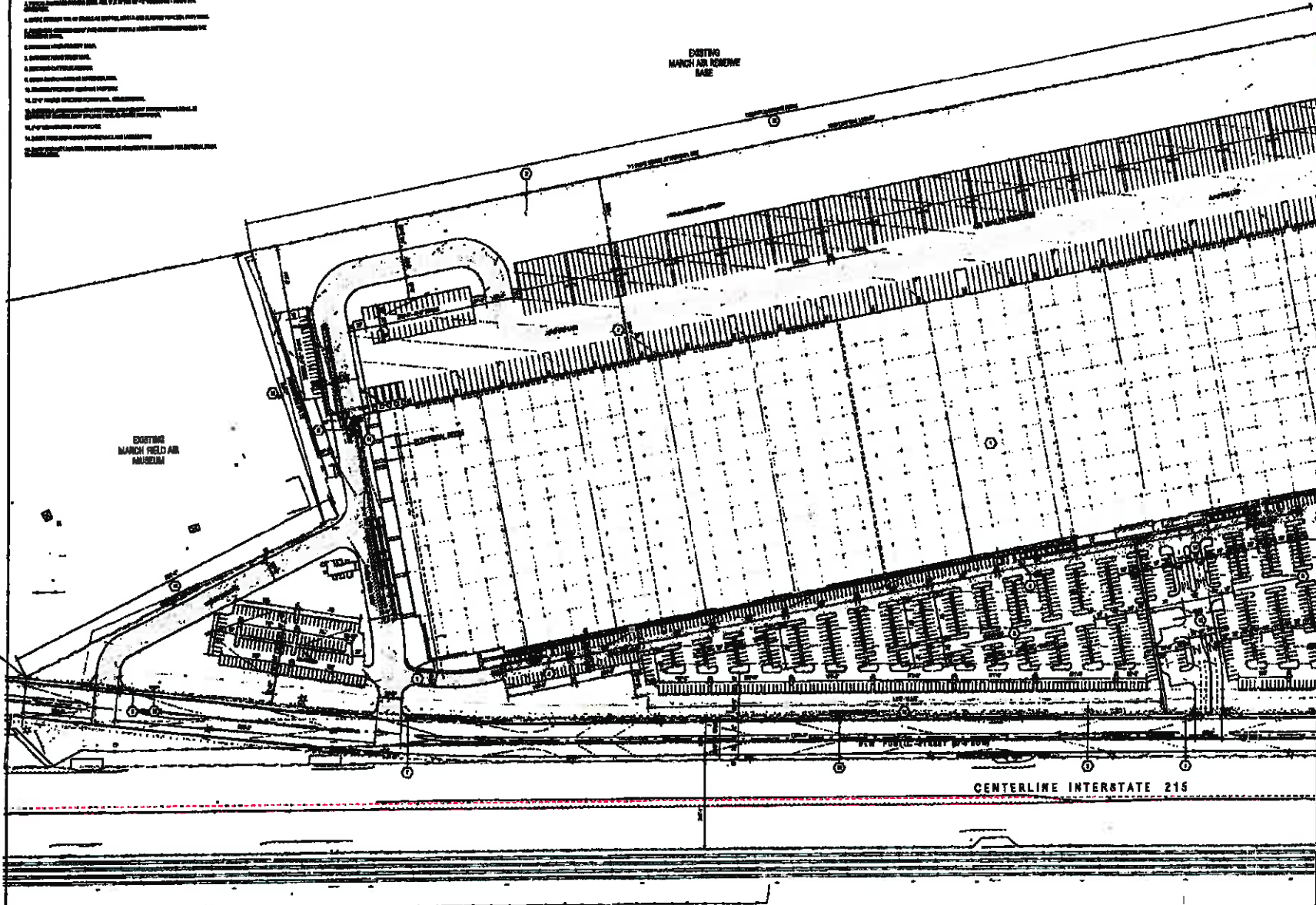
3. THE PROJECT IS SUBJECT TO THE PLANNING DEPARTMENT'S ZONING ORDINANCES.

4. THE PROJECT IS SUBJECT TO THE CITY OF RIVERSIDE'S ZONING ORDINANCES.



OVERALL MASTER SITE PLAN  
10/15/04

- KEYNOTES**
- 1. EXISTING BUILDING FOOTPRINT
  - 2. EXISTING DRIVEWAYS
  - 3. EXISTING DRIVEWAY PAVEMENT
  - 4. EXISTING DRIVEWAY SIDEWALK
  - 5. EXISTING DRIVEWAY CURB
  - 6. EXISTING DRIVEWAY LIGHTING
  - 7. EXISTING DRIVEWAY SIGNAGE
  - 8. EXISTING DRIVEWAY FENCE
  - 9. EXISTING DRIVEWAY UTILITY
  - 10. EXISTING DRIVEWAY LANDSCAPE
  - 11. EXISTING DRIVEWAY ASBESTOS
  - 12. EXISTING DRIVEWAY PAVEMENT
  - 13. EXISTING DRIVEWAY SIDEWALK
  - 14. EXISTING DRIVEWAY CURB
  - 15. EXISTING DRIVEWAY LIGHTING
  - 16. EXISTING DRIVEWAY SIGNAGE
  - 17. EXISTING DRIVEWAY FENCE
  - 18. EXISTING DRIVEWAY UTILITY
  - 19. EXISTING DRIVEWAY LANDSCAPE
  - 20. EXISTING DRIVEWAY ASBESTOS



PARTIAL SITE PLAN



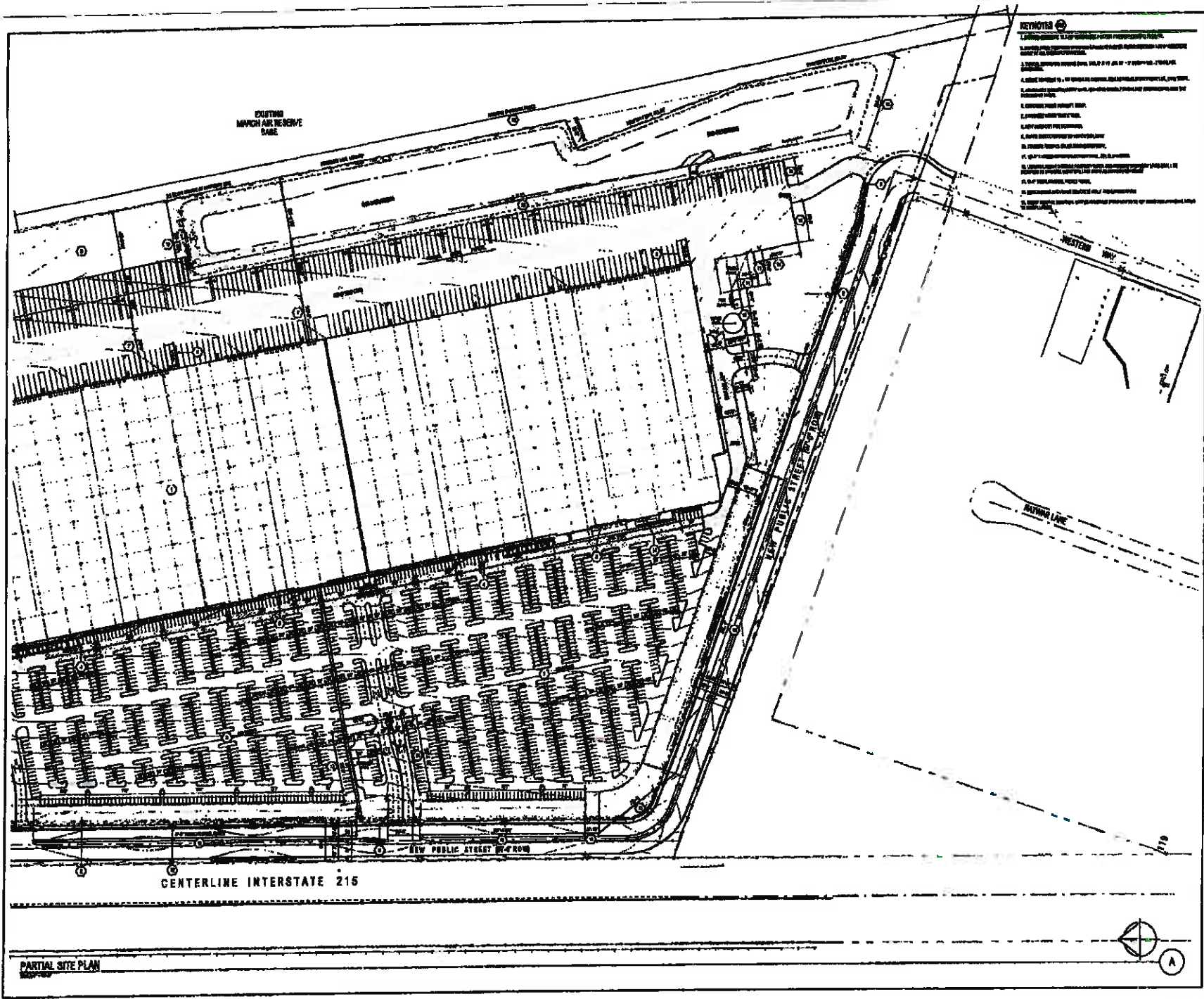
VETERANS INDUSTRIAL  
 PARK 215  
 MIPA,  
 COUNTY OF RIVERSIDE, CA

**HILLWOOD**

RIVERSIDE HILLWOOD  
 DEVELOPMENT, L.L.C.  
 1000 AVENUE OF THE STARS  
 SUITE 1000  
 FALLS CHURCH, VA 22044

NO.	DATE	DESCRIPTION
1	11/10/10	PRELIMINARY PLAN
2	11/10/10	PRELIMINARY PLAN
3	11/10/10	PRELIMINARY PLAN
4	11/10/10	PRELIMINARY PLAN
5	11/10/10	PRELIMINARY PLAN
6	11/10/10	PRELIMINARY PLAN
7	11/10/10	PRELIMINARY PLAN
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9	11/10/10	PRELIMINARY PLAN
10	11/10/10	PRELIMINARY PLAN





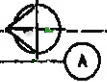
- KEYNOTES**
1. ALL DIMENSIONS ARE IN FEET AND INCHES UNLESS OTHERWISE NOTED.
  2. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED.
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  20. ALL DIMENSIONS ARE TO THE CENTERLINE OF THE UTILITY UNLESS OTHERWISE NOTED.

VETERANS INDUSTRIAL  
PARK 215  
M.P.A.  
COUNTY OF RIVERSIDE, CA

**NEW HILLWOOD**  
A Hill Group Company  
RIVERSIDE ISLAND  
DEVELOPMENT, LLC  
1000 W. FARMERS, SUITE 100  
ONTARIO, CA 91764  
(951) 835-1111

NO.	REVISION	DATE
1	ISSUED FOR PERMITTING	11/15/11
2	ISSUED FOR PERMITTING	11/15/11
3	ISSUED FOR PERMITTING	11/15/11
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20	ISSUED FOR PERMITTING	11/15/11

PARTIAL SITE PLAN  
11/15/11





DATE	
DESCRIPTION	

VETERANS INDUSTRIAL PARK 215  
 M/JPA  
 COUNTY OF RIVERSIDE, CA

**HILLWOOD**  
 A TRUST COMPANY  
 RIVERSIDE ISLAND DEVELOPMENT, LLC  
 200 W. UNIVERSITY AVENUE, 15TH FLOOR  
 RIVERSIDE, CA 92503  
 TEL: 951-514-1100  
 FAX: 951-514-1101

NO.	DATE	DESCRIPTION
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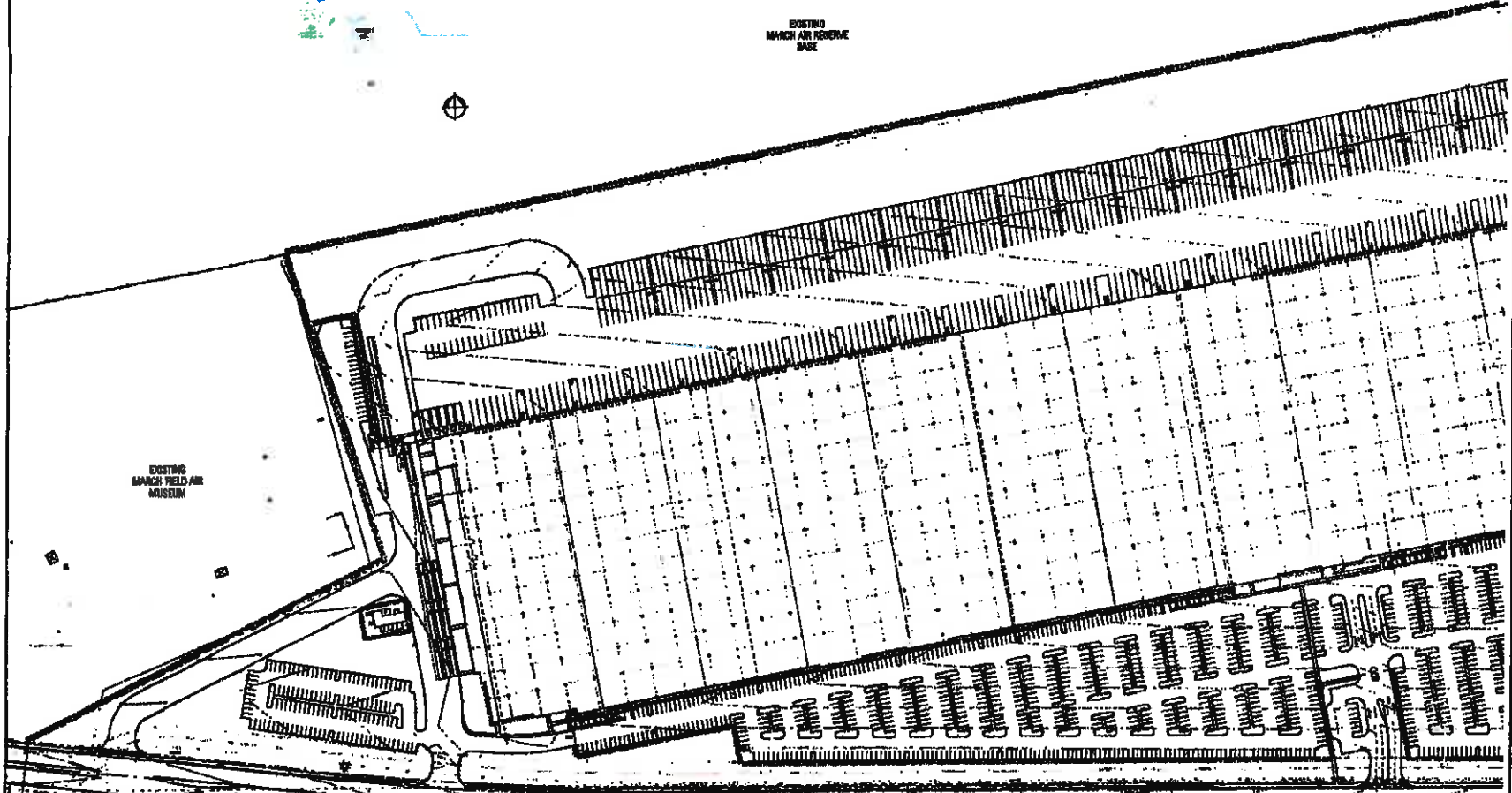
**LEGEND**

EXISTING FENCE LINE

EXISTING MARCH FIELD AIR MUSEUM

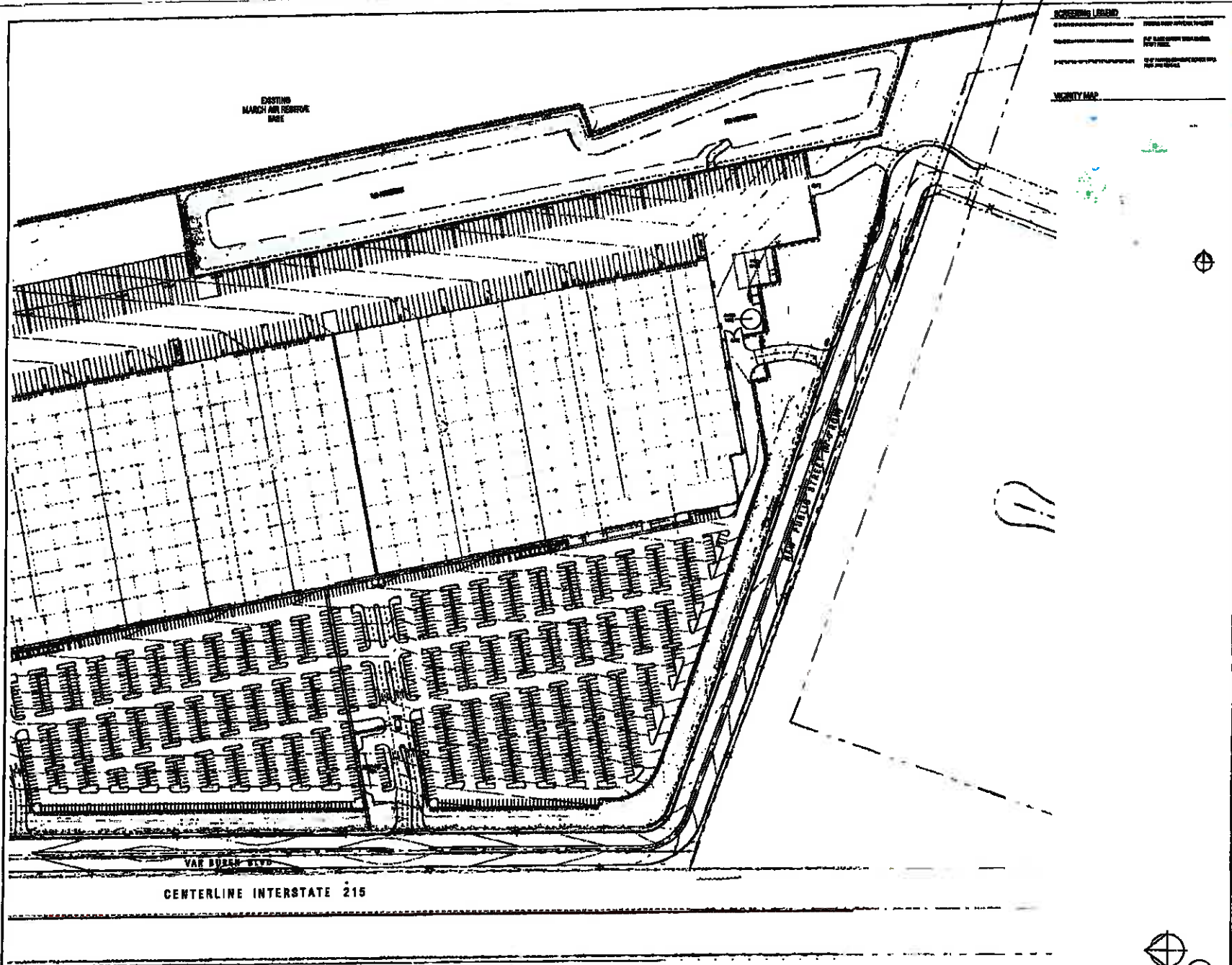
EXISTING MARCH AIR RESERVE BASE

**COUNTY MAP**



PARTIAL SITE PLAN - FENCE PLAN





**SCREENING LEGEND**

- EXISTING IMPROVEMENTS
- PROPOSED IMPROVEMENTS
- PROPOSED UTILITIES

**VELOCITY MAP**



NO. 1

NO.	DESCRIPTION	DATE
1	PRELIMINARY	11/14/11
2	REVISED	11/14/11
3	REVISED	11/14/11
4	REVISED	11/14/11
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9	REVISED	11/14/11
10	REVISED	11/14/11

VETERANS INDUSTRIAL PARK 215  
M.I.P.A.  
COUNTY OF RIVERSIDE, CA

**HILLWOOD**  
RIVERSIDE ISLAND DEVELOPMENT, LLC  
301 W. RIVERSIDE BLVD. STE. 116  
ONTARIO, CA 91764  
(951) 836-1111 FAX

NO.	DATE	DESCRIPTION	BY	CHKD.
1	11/14/11	PRELIMINARY	WJW	WJW
2	11/14/11	REVISED	WJW	WJW
3	11/14/11	REVISED	WJW	WJW
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9	11/14/11	REVISED	WJW	WJW
10	11/14/11	REVISED	WJW	WJW

SCALE: AS SHOWN  
DATE: 11/14/11  
PROJECT: VETERANS INDUSTRIAL PARK 215  
DRAWING: PARTIAL SITE PLAN - FENCE PLAN

PARTIAL SITE PLAN - FENCE PLAN  
DATE: 11/14/11







**LOCAL DESCRIPTION**

THE LOCAL DESCRIPTION OF THIS PARCEL MAP WAS PREPARED BY THE ENGINEER OF RECORD FOR THE PURPOSE OF SHOWING THE AREA TO BE DESCRIBED IN THIS PARCEL MAP AND IS SUBJECT TO THE LOCAL DESCRIPTION OF THIS PARCEL MAP.

THIS PARCEL MAP SHOWS THE LOCATION OF THE PARCEL MAP IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AND IS SUBJECT TO THE LOCAL DESCRIPTION OF THIS PARCEL MAP.

THE LOCAL DESCRIPTION OF THIS PARCEL MAP IS AS FOLLOWS: THE PARCEL MAP SHOWS THE LOCATION OF THE PARCEL MAP IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AND IS SUBJECT TO THE LOCAL DESCRIPTION OF THIS PARCEL MAP.

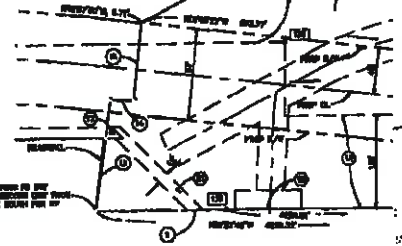
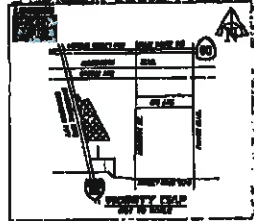
**RESERVATION REQUIREMENTS**

- 1. ALL RIGHTS RESERVED BY THE STATE OF CALIFORNIA AND THE COUNTY OF RIVERSIDE IN THE PARCEL MAP ARE RESERVED BY THE ENGINEER OF RECORD.
- 2. THE ENGINEER OF RECORD HAS CONDUCTED A VISUAL SURVEY OF THE PARCEL MAP AND HAS FOUND THAT THE PARCEL MAP IS CORRECTLY DESCRIBED.
- 3. THE ENGINEER OF RECORD HAS CONDUCTED A VISUAL SURVEY OF THE PARCEL MAP AND HAS FOUND THAT THE PARCEL MAP IS CORRECTLY DESCRIBED.

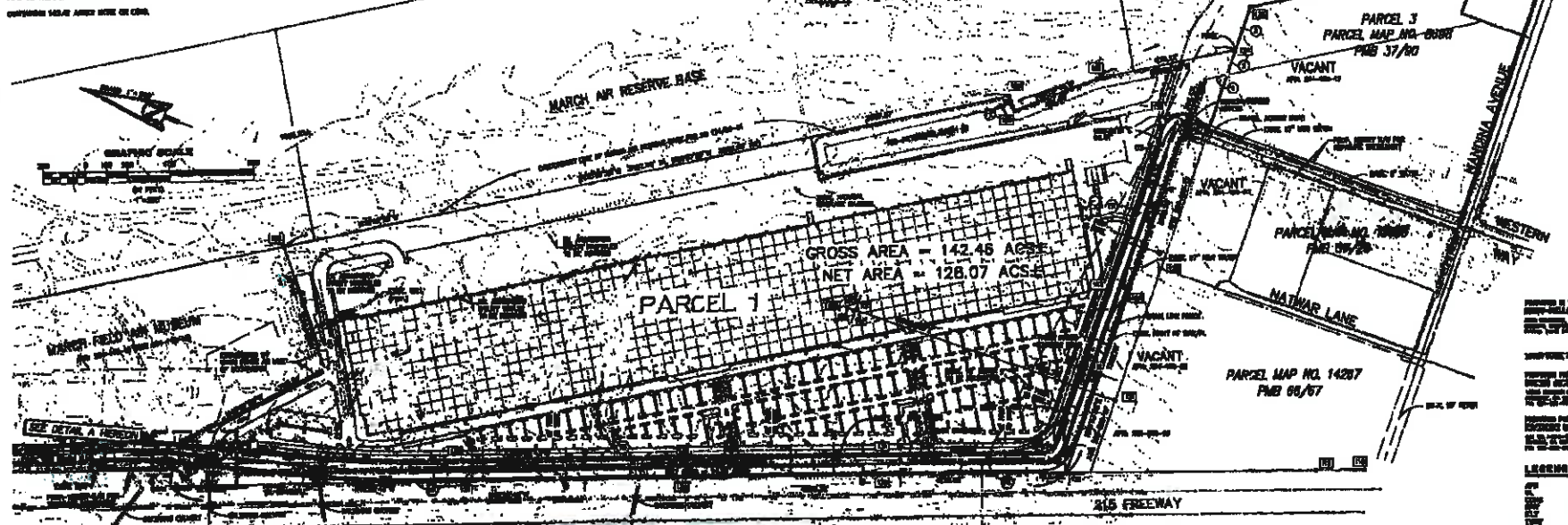
**TENTATIVE PARCEL MAP NO. 37220**

COUNTY OF RIVERSIDE, STATE OF CALIFORNIA  
**HUIT-ZOLIARS, INC.**  
FEBRUARY, 2000

GROSS AREA = 142.46 ACRES  
NET AREA = 128.07 ACRES  
APN'S: 284-130-006, 284-170-005, 284-180-036, 284-140-113, 285-300-028



APPROXIMATE CL. OF RUNWAY



**GENERAL NOTES**

- 1. THE ENGINEER OF RECORD HAS CONDUCTED A VISUAL SURVEY OF THE PARCEL MAP AND HAS FOUND THAT THE PARCEL MAP IS CORRECTLY DESCRIBED.
- 2. THE ENGINEER OF RECORD HAS CONDUCTED A VISUAL SURVEY OF THE PARCEL MAP AND HAS FOUND THAT THE PARCEL MAP IS CORRECTLY DESCRIBED.

**PROPERTY DESCRIPTIONS**

- 1. THE PROPERTY IS LOCATED IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA.
- 2. THE PROPERTY IS LOCATED IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA.

**LEGEND**

- 1. EASEMENT
- 2. EASEMENT
- 3. EASEMENT

**KEY TO PARCEL MAP**

- 1. PARCEL MAP NO. 37220
- 2. PARCEL MAP NO. 37220

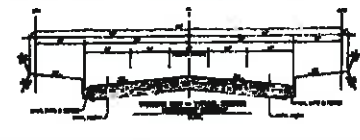


Table with 4 columns: Code, Description, Units, and Remarks. It lists various codes used in the map and their corresponding descriptions.

NOTICE TO THE PUBLIC: THIS PARCEL MAP IS TENTATIVE AND IS SUBJECT TO THE LOCAL DESCRIPTION OF THIS PARCEL MAP.

LEGEND: A list of symbols and their meanings used in the map.

REFERENCES: A list of other maps and documents related to this parcel map.

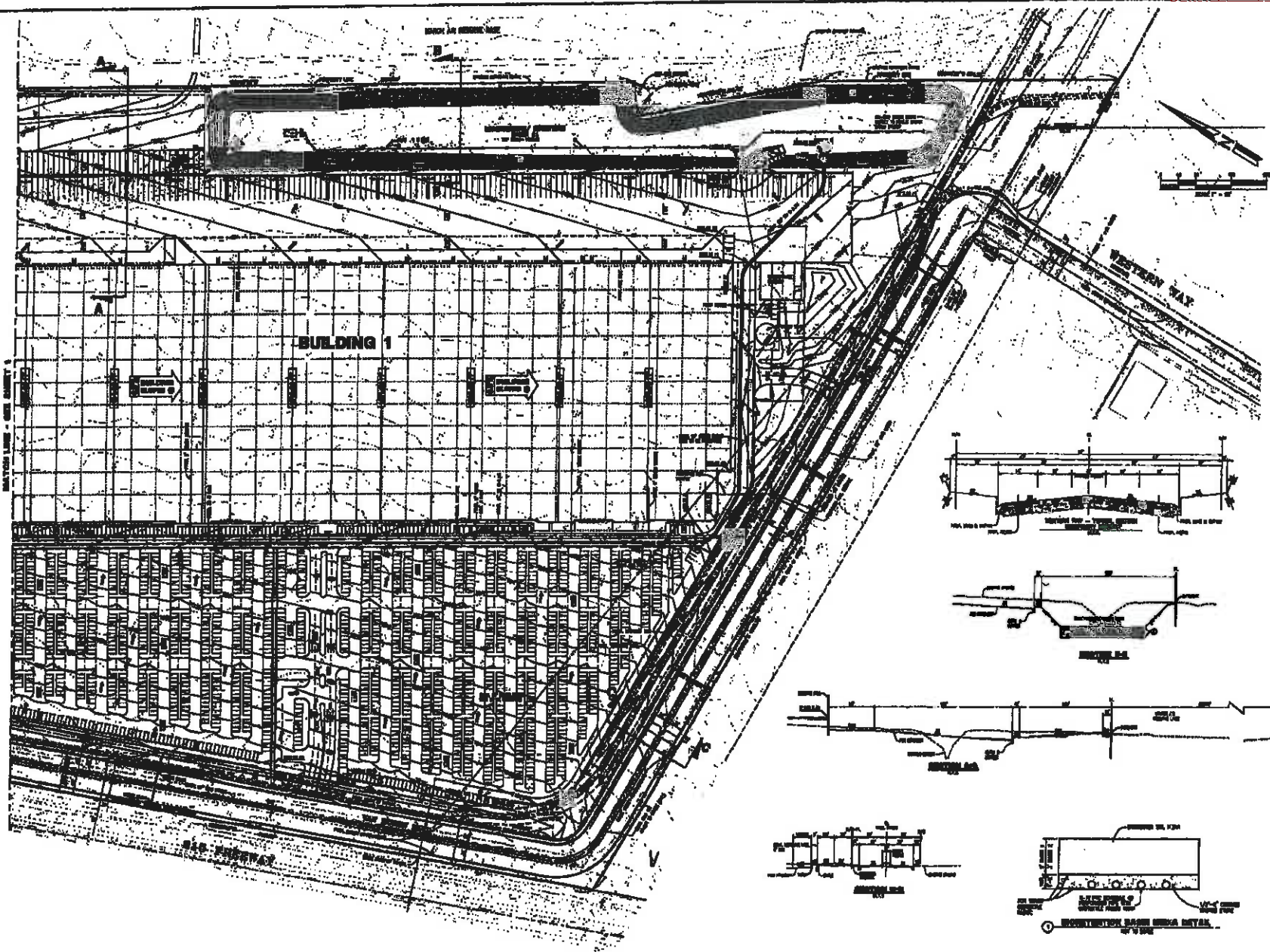
Table with 3 columns: Code, Description, and Units. It provides a key for the symbols used in the map.

TENTATIVE PARCEL MAP 37220 FOR VETERANS INDUSTRIAL PARK 215-PROJECT APOLLO EAST OF 215 FREEWAY COUNTY OF RIVERSIDE

HUIT-ZOLIARS logo and contact information, including address and phone number.







**CONCEPTUAL GRADING & UTILITY PLAN**  
 FOR  
**VETERANS INDUSTRIAL PARK 218**  
 PROJECT SPOLLO  
 SPA 18-01, SP 18-02, PP 18-03, TPN 18-04

**HUNT-ZOLIARS**

DATE	12/18/18
BY	JLH
CHECKED	JLH
SCALE	AS SHOWN
PROJECT	VETERANS INDUSTRIAL PARK 218
SHEET	1 OF 1

# Veterans Industrial Park 215

---

SPECIFIC PLAN

January 2020

**DRAFT**

# Veterans Industrial Park 215 Specific Plan (SP-8)

January 2020

**DRAFT**



**Prepared For:**

March JPA

**Applicant:**

Riverside Inland Development, LLC

Kimley»Horn

# Table of Contents

## Veterans Industrial Park 215 Specific Plan

<b>Section</b>		<b>Page</b>
<b>1. Introduction</b>		
1.1	Location and Access.....	1-1
1.2	Background and History.....	1-1
1.3	Context and Local Setting .....	1-3
	1.3.1 Specific Plan Area and Ownership .....	1-3
	1.3.2 Existing General Plan and Zoning .....	1-3
	1.3.3 Existing and Surrounding Uses .....	1-3
	1.3.4 Existing Infrastructure .....	1-5
	1.3.5 Airport Constraints.....	1-5
	1.3.6 Legal Context.....	1-6
1.4	Discretionary Actions .....	1-7
1.5	Plan Organization.....	1-8
<b>2. DEVELOPMENT PLAN</b>		
2.1	Land Use.....	2-1
	2.1.1 Project Objectives .....	2-1
	2.1.2 Land Use Plan .....	2-2
	2.1.3 Land Use Compatibility .....	2-5
2.2	Circulation Plan .....	2-6
	2.2.1 Regional.....	2-6
	2.2.2 Arterial Highways .....	2-6
	2.2.3 Site Access .....	2-7

2.2.4	Parking.....	2-7
2.3	Infrastructure and Services .....	2-9
2.3.1	Water Service .....	2-9
2.3.2	Wastewater Service .....	2-10
2.3.3	Storm Water Management.....	2-13
2.3.4	Grading.....	2-16
2.3.5	Dry Utilities.....	2-16
2.3.6	Solid Waste .....	2-16
2.3.7	Police and Fire Service .....	2-16

### **3. DEVELOPMENT REGULATIONS**

3.1	General Provisions .....	3-1
3.1.1	Applicability.....	3-1
3.1.2	Severability.....	3-1
3.1.3	Consistency with Specific Plan .....	3-1
3.1.4	Subdivision Map Act.....	3-2
3.1.5	Determination of Unlisted Use .....	3-2
3.1.6	Interpretation .....	3-2
3.1.7	Definitions .....	3-2
3.1.8	Design Guidelines.....	3-3
3.1.9	March Air Reserve Base Performance Standards .....	3-3
3.2	Permitted Uses.....	3-4
3.2.1	Permitted Uses.....	3-4
3.2.2	Ancillary Uses.....	3-4
3.2.3	Conditional Uses subject to further Environmental Review.....	3-4
3.2.4	Prohibited Uses .....	3-4

3.3	Development Standards .....	3-5
3.3.1	Parking .....	3-6
3.3.2	General Design Standards.....	3-6

#### **4. DESIGN GUIDELINES**

4.1	Introduction .....	4-1
4.2	Architecture Guidelines and Standards .....	4-2
4.2.1	Building Form and Orientation .....	4-2
4.2.2	Materials and Colors.....	4-4
4.2.3	Windows and Doors.....	4-4
4.2.4	Loading Docks and Service Doors .....	4-4
4.2.5	Security Elements .....	4-5
4.2.6	Trash Enclosures .....	4-5
4.3	Landscape Guidelines .....	4-5
4.3.1	Landscape Master Plan .....	4-5
4.3.2	Water Quality.....	4-6
4.3.3	Utility Placement and Screening.....	4- <del>1110</del>
4.3.4	Walls and Fences .....	4- <del>1110</del>
4.3.5	Exterior Lighting.....	4- <del>1311</del>
4.3.6	Signage .....	4- <del>1312</del>

#### **5. ADMINISTRATION AND IMPLEMENTATION**

5.1	Administration .....	5-1
5.1.1	Responsibility .....	5-1
5.1.2	Applicability .....	5-1
5.1.3	Enforcement and Interpretation .....	5-1
5.1.4	Severability .....	5-1

5.1.5	Initial Entitlements .....	5-1
5.1.6	Administrative Amendments .....	5-2
5.1.7	Amendments.....	5-3
5.1.8	Appeals.....	5-3
5.2	Implementation .....	5-3
5.2.1	Adoption .....	5-3
5.2.2	Phasing.....	5-3
5.2.3	Maintenance and Ownership .....	5-4
5.2.4	Relationship to CEQA.....	5-4

## 6. APPENDICES

- A Legal Description
- B General Plan Conformance
- C Airport Compatibility Plan
- D Landscaping Near Airports

## List of Figures

<b>Figure</b>		<b>Page</b>
1-1	Regional Location .....	1-2
1-2	Specific Plan Area .....	1-4
2-1	Conceptual Land Use .....	2-3
2-2	Conceptual Site Plan.....	2-4
2-3	Van Buren Extension Typical Cross Section.....	2-6
2-4	Western Way Typical Cross Section .....	2-7
2-5	Circulation Plan.....	2-8
2-6a	Conceptual Water Plan.....	2-11
2-6b	Conceptual Sewer Plan.....	2-12
2-7	Drainage Plan .....	2-15
2-8	Conceptual Grading Plan.....	2-17
4-1	Example Architecture .....	4-3
4-2	Van Buren Boulevard Streetscape Edge.....	4-6
4-3	Planning Area 1 Landscape Plan.....	4-8
4-4	Planning Area 2 Landscape Plan.....	4-9

## List of Tables

<b>Table</b>		<b>Page</b>
1-1	Requested Approvals .....	1-8
2-1	Land Use .....	2-5
2-2	Service Providers .....	2-9
3-1	Development Standards.....	3-5
3-2	Parking.....	3-5
4-1	Plant Materials .....	4-7
<del>4-2</del>	<del>Bieretention Basin Seed Mix .....</del>	<del>4-11</del>
5-1	Financing, Ownership and Maintenance.....	5-4



# **1** INTRODUCTION

**This section explains the purpose of the specific plan; local and regional context and setting; background; planning process and entitlements; authority to prepare; relationship to existing plans and policies; and organization of the specific plan.**

## 1.1 LOCATION AND ACCESS

The Veterans Industrial Park 215 Specific Plan area encompasses approximately 142.5 acres of airport property consisting of Parcel D2 within the boundaries of the March Inland Port Airport, located in Riverside County, California. It is located in the East March Planning Subarea. Parcel D2 is located directly east of the I-215 off-ramp at Van Buren Boulevard, south of the existing March Field Air Museum and west of an existing airport runway; but, provides no access to the runway or any taxiways (flying facilities).

Access to Parcel D2 is provided via the I-215 freeway and Van Buren Boulevard. A southern extension of Van Buren Boulevard is anticipated as a part of this Specific Plan. Figure 1-1, *Location*, shows the regional location of the Specific Plan area.

## 1.2 BACKGROUND AND HISTORY

March Air Force Base (MAFB) was first established as a military installation in 1918 and has been in near continuous operation between 1918 and 1993. In 1993 the federal government called for the realignment of MAFB and a substantial reduction in its military use. In April 1996, March Air Force Base was re-designated as an Air Reserve Base (ARB). The conversion of MAFB to an air reserve base resulted in the need to dispose of and reuse approximately 4,400 acres of land. In order to limit the economic disruption caused by base closures, the California State Legislature authorized the formation of joint powers authorities to regulate the redevelopment of closed/realigned military installations. The cities of Moreno Valley, Perris, the City of Riverside, and the County of Riverside formed the March Joint Powers Authority (MIPA) pursuant to Article 1, Chapter 5, Division 7, Title 1 (commencing with Section 6500 et seq.). The March JPA was delegated the authority to manage the use, reuse, and joint use of the realigned base.

Since 1996, the MIPA has prepared a number of planning, policy and regulatory documents to guide the redevelopment of the former MAFB. These documents, that impact the Specific Plan area include:

- Final Environmental Impact Statement: Disposal of Portions of March Air Force Base (February 1996)
- Final Environmental Impact Report for the March Air Force Base Redevelopment Project (June 1996)
- Department of Defense Instructions
- Air Force Instructions
- General Plan of the March Joint Powers Authority (September 1999)
- March Joint Powers Authority Development Code (July 1997)
- Master Environmental Impact Report for the General Plan of the March Joint Powers Authority (September 1999)
- Air Installation Compatible Use Zone Study for March Air Reserve Base (2018)
- March Air Reserve Base/Inland Port Airport Joint Land Use Study (December 2010)
- March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan (November 2014)
- Airport Layout Plan (September 2013)



Figure 1-1 Regional Location

In addition to these documents related to development within the airport area, the Perris Valley Commerce Center Specific Plan (PVCCSP) was adopted by the City of Perris in January 2012. The PVCCSP consists of approximately 3,500 acres located immediately south of the Veterans Industrial Park 215 Specific Plan area. Western Avenue, an arterial roadway within the PVCCSP, will be extended to the southern edge of the Specific Plan area as a secondary point of access.

Riverside Inland Development, LLC is the Master Developer for the remaining MIPA owned properties at the March Inland Port. Hillwood entered into an Exclusive Negotiating Agreement (ENA) in December 2015 in response to a Request for Proposal and subsequently entered into a Memorandum of Agreement. The development program addressed by this Specific Plan is the first implementing step of these agreements.

### **1.3 CONTEXT AND LOCAL SETTING**

#### **1.3.1 Specific Plan Area and Ownership**

The Specific Plan area is comprised of approximately 142.5 gross acres of vacant land. The project site is composed of five tax parcels, Assessor Parcel Numbers (APNs) 294-150-009, 294-170-005, 295-300-008, 294-140-13 and 294-180-038. The property is presently owned by the March Joint Powers Authority. Figure 1-2 illustrates the Specific Plan area.

#### **1.3.2 Existing General Plan and Zoning**

The Specific Plan area consists of approximately 142.5 acres of land that is not currently zoned. Its General Plan designation is Aviation, with an allowable Floor Area Ratio (FAR) of 0.4. This property is expected to be developed under a long-term ground lease.

As part of project entitlements, the property would be subject to a General Plan Amendment that would add a Specific Plan overlay ("SP") to the existing Aviation designation. In addition, the Veterans Industrial Park 215 Specific Plan would be adopted as the zoning for the property. An analysis of compliance with General Plan goals and policies may be found in the Appendix of this Specific Plan document.

#### **1.3.3 Existing and Surrounding Uses**

The property is presently vacant. The 142.5-acre Specific Plan area is surrounded by the following uses:

**North:** Immediately to the north of the development parcel is the existing March Field Air Museum, zoned for Public Facilities uses.

**East:** Property to the east consists of the existing runways of March Air Reserve Base.

**South:** Immediately south of the parcel is the corporate boundary of the City of Perris, and the Perris Valley Commerce Center Specific Plan, zoned for Business Park and Light Industrial uses.

**West:** West of the parcel is I-215, with the Riverside National Cemetery and Meridian Specific Plan area beyond.



Figure 1-2 Specific Plan Area

### 1.3.4 Existing Infrastructure

#### Existing Roadways

There is currently no improved roadway access to the Specific Plan area.

Regional access to the Specific Plan area is provided by Interstate 215 (I-215). Interstate 215 runs north/south and is immediately to the west of the Specific Plan area. The nearest freeway ramps are approximately ¼ mile north of the Specific Plan area at Van Buren Boulevard.

Van Buren Boulevard is a primary east/west corridor. It provides access to I-215, and is currently designated as an Arterial Highway to the west of I-215. On the east side of I-215 Van Buren Boulevard is currently planned as a Major Arterial with two northbound lanes, two southbound lanes and a raised landscaped median. It is constructed consistent with this standard for approximately 1,400 feet along the frontage of the March Field Air Museum, although the street's median is painted rather than raised. Van Buren Boulevard currently terminates approximately 300 feet to the north of the Specific Plan area.

#### Existing Infrastructure

- **Water.** The Specific Plan Area is located within the Riverside retail service area of the Western Municipal Water District (WMWD). Existing Eastern Municipal Water District (EMWD) 8-inch water lines are present in Nandina Avenue and Western Way and a 36-inch water line is located in Harley Knox to the south of the Specific Plan Area in the City of Perris. In addition, an existing 12-inch WMWD water line is present near the northern edge of the Specific Plan Area, within the March Air Museum property.

The U. S. Air Force maintains an existing 354-foot deep groundwater monitoring well, located within the property near the northern boundary with the Air Museum.

- **Sewer Service.** Sewer service in the Specific Plan area is provided by Western Municipal Water District (WMWD). Existing EMWD 10- and 12-inch sewer lines are present to the south of the project in Nandina Avenue in the City of Perris however these lines will not be utilized by the project. In addition, a 10-inch WMWD sewer force main crosses the site's southern edge, continuing west across the I-215 freeway.
- **Drainage.** An existing earthen channel runs from the site's northwest corner to its southeast corner, and a second drainage course is present which runs in an east-west direction, connecting with the earthen channel. This channel collects off-site storm water from four culverts beneath I-215 and conveys it south through the airport property. This earthen channel conveys runoff southerly towards Heacock Street and discharges into Perris Valley Channel in the City of Perris, ultimately discharging to the San Jacinto River, Canyon Lake, and Lake Elsinore.

### 1.3.5 Airport Constraints

The March JPA has full land use authority over portions of the former base under its direct control. In order to address airport land use compatibility issues around the March Air Reserve Base/Inland Port, the Riverside County Airport Land Use Commission prepared and adopted a Land Use Compatibility Plan (March ARB/IPA ALUCP) which serves as the JPA's land use compatibility planning recommendations. In

addition, the Department of Defense Instructions (DDI) and the Air Force Instructions (AFI) apply. The runway system and military areas of the airport are under the control of the U.S. Air Force. There are two active runways at March ARB/IP, Runway 14-32, and Runway 12-30. Runway 14-32, the airport's primary runway, is 13,300 feet long and 200 feet wide. Runway 14-32 has a standard left traffic pattern which means all turns in the traffic pattern departing north are made to the west. The primary runway (Runway 14-32) is located immediately east of the D2 parcel and the Specific Plan area.

The Land Use Compatibility Plan (RCALUP) establishes a boundary for the influence area of March ARB/IPA, related to noise, overflight, safety, and airspace protection. The Specific Plan Area:

- Falls between the 65 and 75 CNEL noise contours of the airport.
- Does not fall within any of the accident zones of the JLUS (Clear Zone or Accident Potential Zones I or II).

A number of regulations and land use/height restrictions affect the Specific Plan area. The property is located within the 7:1 Transitional Surface area of the March Air Reserve Base, and a 35-foot building height limitation line extends along the runway edge as identified on the Airport Layout Plan.

### 1.3.6 Legal Context

A "Specific Plan" is a planning and regulatory tool made available to local governments by the State of California. Specific plans implement an agency's General Plan through the development of policies, programs, and regulations that provide an intermediate level of detail between General Plans and individual development projects. State law stipulates that specific plans can only be adopted or amended if they are consistent with an adopted General Plan.

The Veterans Industrial Park 215 Specific Plan implements the goals and policies of the General Plan, serves as an extension of the General Plan, and can be used as both a policy and a regulatory document. The purpose of this Specific Plan is to implement the vision by providing goals, policies, programs, development standards, and design guidelines to direct future development within the Specific Plan Area.

The authority to prepare and adopt a Specific Plan and the requirements for its contents are set forth in California Government Code Sections 65450 through 65457. Section 65451 states:

*A Specific Plan shall include a text and a diagram or diagrams which specify all of the following in detail:*

- *The distribution, location and intent of the uses, including open space, within the area covered by the plan.*
- *The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described by the plan.*
- *Standards and criteria by which the development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable.*

- *A program of implementation measures including programs, public works projects, and financing measures.*
- *The Specific Plan shall include a statement of the relationship of the Specific Plan to the General Plan.*

#### 1.4 DISCRETIONARY ACTIONS

The following discretionary actions will be required in conjunction with the proposed Veterans Industrial Park 215 Specific Plan:

**California Environmental Quality Act.** This Specific Plan is considered a Project under the California Environmental Quality Act (CEQA). CEQA is a statute that requires state and local agencies to identify the significant environmental impacts of their actions and to avoid or mitigate those impacts, if feasible. To document the potential significant impacts an Environmental Impact Report (EIR) will be prepared for this Specific Plan and must be certified by the Commission prior to adoption of this Specific Plan or any other project entitlements. Subsequent development within the Veterans Industrial Park 215 Specific Plan boundaries deemed consistent with said Specific Plan standards will not require further environmental review except as specified in the Development Regulations section of this document (Section 3). March JPA will be the lead agency responsible for certification of the Project's EIR.

**General Plan Amendment.** A General Plan Amendment to attach a Specific Plan overlay to the Specific Plan Area. The General Plan Amendment will be adopted by resolution. ~~The existing General Plan land use designation of Aviation would be expanded to include general warehousing and logistic uses.~~

**Specific Plan/Zone Change.** The Specific Plan Area is presently un-zoned. The establishment of the Specific Plan for the proposed project will provide for an SP-8 designation on the March JPA Zoning Map ~~with an underlying Aviation Designation.~~ Adoption of this Specific Plan as part of a zone change is a discretionary action subject to March JPA approval. Adopted by Ordinance, the Specific Plan document will serve both planning and regulatory functions. This document contains the development standards and procedures necessary to fulfill these purposes.

**Tentative Parcel Map.** The Specific Plan Area is comprised of five assessor parcels for taxation purposes but is not presently a legal development parcel. The proposed project will include a Tentative Parcel Map to create two legal development parcels, dedicate rights-of-way for the extension of Van Buren and Western Avenues and identify required utility easements.

**Plot Plan.** A site development plan for the project, consisting of an Industrial/logistics project with proposed structures, parking, landscaping, drainage facilities, and new streets and driveways.

**Development Agreement (DA) and Disposition and Development Agreement (DDA).** A statutory development agreement, authorized pursuant to California Government Code Section 65864 et seq., will be processed as part of the approval of this Specific Plan. The development agreement of this Specific Plan will include, among other items, methods for financing acquisition and construction of infrastructure,



and phasing, including future phasing. Such development agreement shall be fully approved before the issuance of the first building permit for this project.

**Avigation Easement.** Development projects must provide an executed easement to the MJPA prior to a final map or building permit.

Requested Permit/Approval	Approving Agency
Final EIR Certification	March JPA
Specific Plan Adoption	March JPA
General Plan Amendment	March JPA
Development Agreement	March JPA
Tentative Parcel Map Approval	March JPA
Plot Plan Approval	March JPA
Water Supply Assessment	Western Municipal Water District
Encroachment Permit (Drainage)	Caltrans
Encroachment Permit (Western Way)	City of Perris
1602 Permit	California Department of Fish and <del>Wildlife</del> Game
404 Permit <del>and associated EA</del>	U.S. Army Corps of Engineers
401 Permit	Regional Water Quality Control Board
NPDES	Regional Water Quality Control Board

## 1.5 PLAN ORGANIZATION

### Section 1 – Introduction

This section explains the purpose of the Specific Plan; local and regional context and setting; background; planning process and entitlements; guiding principles; authority to prepare; relationship to existing plans and policies; and organization of the Specific Plan.

### Section 2 – Development Plan

This section explains the conceptual land use plan for the Specific Plan Area; identifies land use policies, and defines the land use designations unique to the Specific Plan. The circulation, drainage, water and sewer, grading, and public services plans are also described.

### Section 3 – Development Regulations

This section explains the development standards for the land use designations established in the Development Plan, including the standards for allowable uses, setbacks, parking, and signage.

### Section 4 – Design Guidelines

This section explains design concepts and establishes design guidelines for development in the Specific Plan Area.

**Section 5 – Administration and Implementation**

This section discusses the development review procedures by the JPA and other relevant permitting agencies, applicable to the Specific Plan Area. Implementation of the proposed land uses, including Specific Plan adoption, subsequent approvals and plans, substantial conformance findings, and phasing are outlined in this chapter. Additionally, financing sources and maintenance responsibilities are identified.

# 2

## DEVELOPMENT PLAN

**This chapter explains the various elements of the Veterans Industrial Park 215 Specific Plan, including Land Use, Circulation, Public Services, and Infrastructure.**

## 2.1 LAND USE

This section of the Veterans Industrial Park 215 Specific Plan discusses the components of the Specific Plan such as land use, circulation, grading, drainage, water and sewer utilities, and public services.

Each of these components is discussed in further detail in the sections below.

### 2.1.1 Project Objectives

The proposed Veterans Industrial Park 215 Specific Plan is intended to achieve the following objectives:

- Develop and operate a state-of-the-art logistics center that takes advantage of existing and planned March JPA infrastructure, is feasible to construct, and is economically competitive with, and in the general vicinity of similar industrial logistics and distribution center uses.
- Develop and operate a large format logistics center that is in close proximity to the former March Air Reserve Base and I-215/State Route 60 to support the distribution of goods throughout the region and that also limits truck traffic disruption to sensitive receptors within the surrounding region.
- Develop and operate a large format logistics center that will enhance e-commerce opportunities, and attract quality tenants and will be competitive with other similar facilities in the region.
- Maximize efficient goods movement throughout the region by locating a large format logistics center in close proximity to the Ports of Los Angeles and Long Beach thereby enabling trucks servicing the site to achieve a minimum of two roundtrips per day.
- Develop and operate a large format logistics center that maximizes the use of a large industrial site in the region that is in close proximity to the Ports of Los Angeles and Long Beach, to realize substantial unmet demand in the region, allowing the region to compete on a domestic and international scale through the efficient and cost-effective movement of goods.
- Develop and operate a large format logistics center that meets and/or exceeds industry standards for operational criteria, including energy efficiency.
- Facilitate the development of underutilized land currently planned for aviation-related uses with uses that maximize the use of the site as a large format logistics center consisting of one or more buildings with total building space in excess of 2,000,000 square feet in size and responds to market demand within the Veterans Industrial Park 215 Specific Plan and surrounding area.
- Facilitate the establishment of design guidelines and development standards consistent with the March JPA Development Code and that create a unique, well-defined identity for the proposed project.
- Positively contribute to the economy of the region through new capital investment, creation of new employment opportunities, including opportunities for highly-trained workers and replacement jobs for those lost due to military base closures, and expansion of the tax base.
- Provide for the extension of planned roadways consistent with the March JPA Circulation Element.
- Establish landscape guidelines that emphasize the use of drought-tolerant and water-efficient plant materials.

- Establish guidelines for energy efficiency that promote the conservation of energy resources in the construction and operation of the proposed large format logistics center use.
- Provide for off-site realignment of existing drainage channels in order to minimize potential drainage and related impacts associated with the proposed large format logistics center, including impacts to aviation uses at the adjacent runway.
- Identify and provide for water, sewer, drainage, and road facility infrastructure that is required to adequately serve the proposed large format logistics center.
- ~~▪ Develop and operate a state-of-the-art logistics center that takes advantage of existing and planned Marsh Joint Powers Authority ("MIPA") infrastructure, is feasible to construct, and is economically competitive with, and in the general vicinity of, similar industrial, logistics and distribution center uses.~~
- ~~▪ Develop and operate a large format logistics center that is in close proximity to the former March Air Reserve Base and I-215/State Route 60 to support the distribution of goods throughout the region and that also limits truck traffic disruption to sensitive receptors within the surrounding region.~~
- ~~▪ Develop and operate a large format logistics center that may accommodate e-commerce opportunities, attract quality tenants and will be competitive with other similar facilities in the region.~~
- ~~▪ Maximize efficient goods movement throughout the region by locating a large format logistics center in close proximity to the Ports of Los Angeles and Long Beach thereby enabling trucks servicing the site to achieve a minimum of two roundtrips per day.~~
- ~~▪ Develop and operate a large format logistics center that maximizes the use of a large industrial site in the region that is in close proximity to the Ports of Los Angeles and Long Beach, to realize substantial unmet demand in the region, allowing the region to compete on a domestic and international scale through the efficient and cost-effective movement of goods.~~
- ~~▪ Develop and operate a large format logistics center that meets and/or exceeds industry standards for operational criteria, including energy efficiency.~~
- ~~▪ Implement the Veterans Industrial Park 215 Specific Plan through development of an airport land use program that is consistent with the proposed development standards and criteria relevant to the site and proposed large format logistics center use.~~
- ~~▪ Facilitate the development of underutilized land currently planned for aviation-related uses that maximizes the use of the site and responds to market demand within the Veterans Industrial Park 215 Specific Plan area and surrounding region for a large format logistics center.~~
- ~~▪ Facilitate the establishment of design guidelines and development standards consistent with the MIPA Development Code and that create a unique, well-defined identity for the Veterans Industrial Park 215 Specific Plan.~~
- ~~▪ Positively contribute to the economy of the region through new capital investment, creation of new employment opportunities, including opportunities for highly-trained workers and replacement jobs for those lost due to military base closures, and expansion of the tax base.~~

- ~~• Provide for off-site realignment of existing drainage channels in order to minimize potential drainage and related impacts associated with the proposed large-format logistics center, including impacts to aviation uses at the adjacent runway.~~
- ~~• Provide for the extension of planned roadways consistent with the MIPA Circulation Element.~~
- ~~• Identify and provide for water, sewer, drainage, and road facility infrastructure that is required to adequately service the Veterans Industrial Park 215 Specific Plan area for the intended uses.~~
- ~~• Establish landscape guidelines that emphasize the use of drought-tolerant and water-efficient plant materials.~~
- ~~• Establish guidelines for energy efficiency that promote the conservation of energy resources in the construction and operation of the proposed large-format logistics center use.~~

### 2.1.2 Land Use Plan

The Veterans Industrial Park 215 Specific Plan envisions ~~two~~ logistics structures totaling **up to 2,185,618** square feet, with loading docks, truck trailer parking, and associated infrastructure improvements. The primary logistics use would include typical ancillary uses as outlined in the project's Development Regulations, including warehouse, office, and employee support areas such as meeting rooms and break rooms.

The plan, shown in Figure 2-1 *Conceptual Land Use Plan* and Figure 2-2, *Conceptual Site Plan*, provides the overall vision and guide for the development of the site for logistics uses. Table 2-1, *Land Use*, outlines the **maximum-anticipated** build-out for each planning area.

Although the land use plan identifies two planning areas, if a single user is identified who wishes a single building, buildings may be constructed across planning area boundaries so long as the maximum Floor Area Ratio across the entire area does not exceed the maximum Floor Area Ratio (FAR) for the project as a whole. Individual Planning Area square footages **and layout** may vary as part of plot plan review so long as the FAR for the Specific Plan Area as a whole does not exceed the maximum allowable FAR of 0.4. In the event that a single user and/or building are proposed at the plot plan level of review and entitlement, final engineering layouts for water, sewer, dry utilities, and on-site drainage would be expected to be modified to accommodate this scenario.

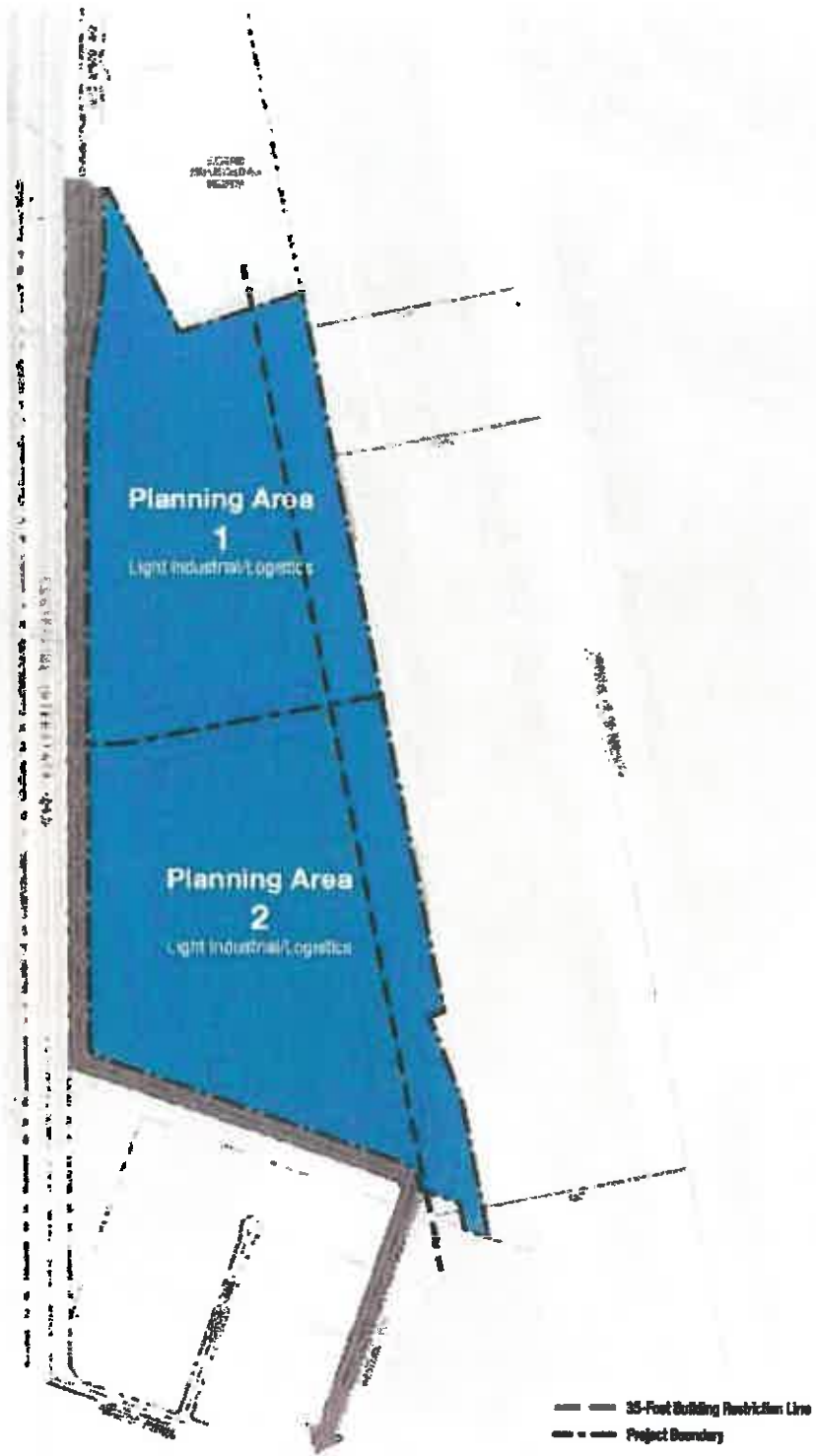
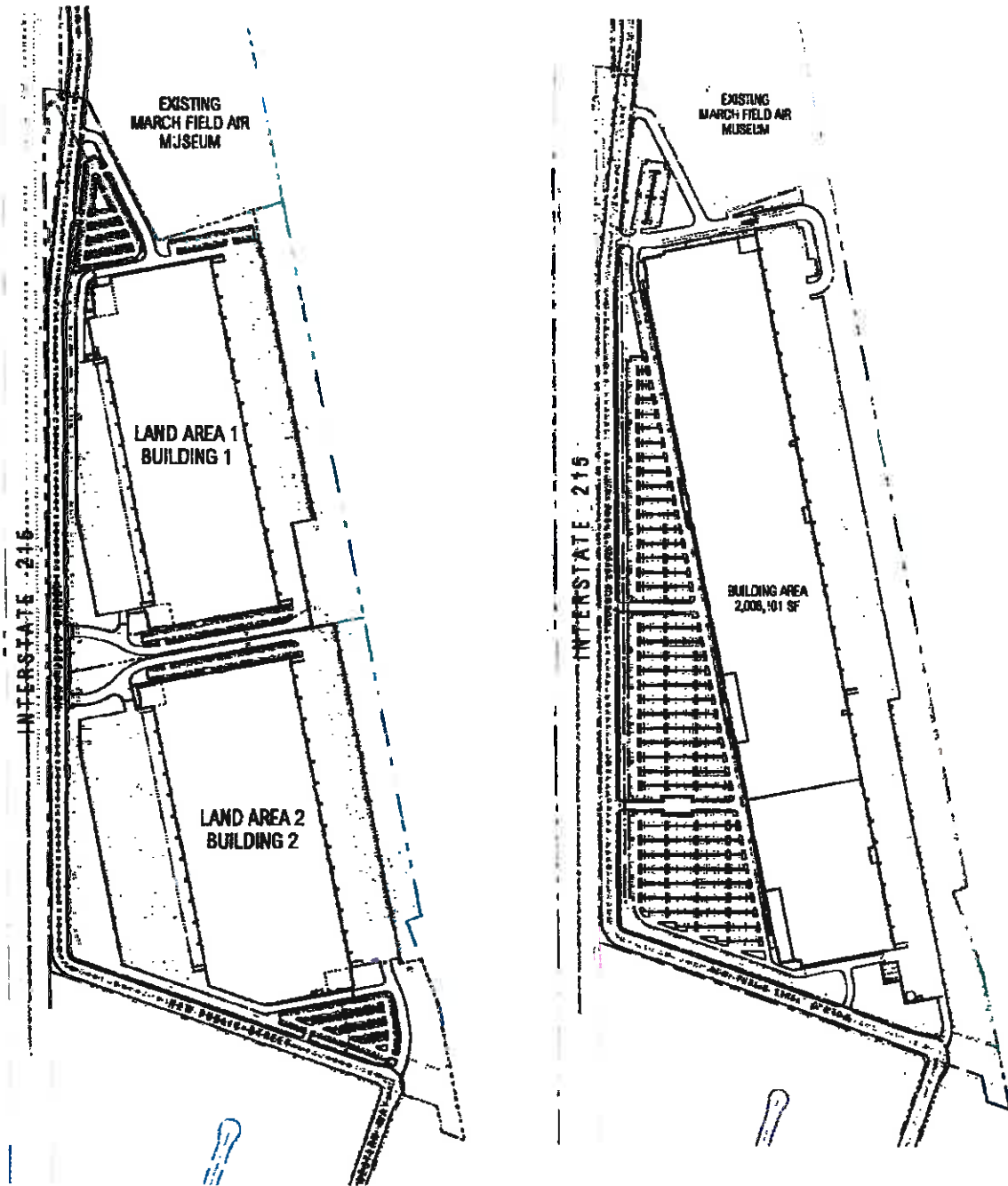


Figure 2-1 Conceptual Land Use



**Two Building Concept**

**Single Building Concept**

Site plan is conceptual only. The ultimate site plan will be subject to site plan review and may differ from this concept.

**Figure 2-2 Conceptual Site Plans**



For purposes of this Specific Plan, if mezzanines are provided as part of a building, only mezzanines providing occupancy (i.e., second floor enclosed office area **if provided**) are counted in the square footage identified in the land use table or included in the maximum FAR.

Planning Area	Land Use	Acres	Building Area (Sq. Ft.)	Floor Area Ratio (FAR)
Planning Area 1/ <del>Building 1</del>	Logistics/Light Industrial	57.47 ac.	<del>1,014,822</del> 1,002,601	0.3 <del>893</del>
Planning Area 2/ <del>Building 2</del>	Logistics/Light Industrial	70.38 ac.	<del>1,170,796</del> 1,087,917	
Road Dedication	—	14.61 ac.	—	—
<b>Total</b>		<b>142.46 ac.</b>	<b>2,090,518</b> <b>2,185,618 maximum</b> <b>(2,227,650 maximum)</b>	<b>0.35-34 (gross)</b> <b>0.388-38 (net)</b> <b>0.4 (maximum)</b>

### 2.1.3 Land Use Compatibility

This Specific Plan is subject to the development restrictions of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan (RCALUP), Department of Defense Instructions (DODI) and Air Force Instructions (AFI). The RCALUP includes nine land use compatibility zones. The aeronautical factors used to establish the compatibility zone boundaries are described below and summarized in Exhibit 3-2, Compatibility Zone Factors (see Appendix B). The Compatibility Map (Exhibit 3-3 in Appendix B) depicts the compatibility zones for March ARB and Inland Port Airport (IPA). These compatibility zones and the factors upon which they are based are similar in concept to the compatibility zones adopted by the Riverside County ALUC for other airports in the county. However, the characteristics of aircraft activity at March ARB/IPA compared to primarily general aviation activity at the other airports in the county required the development of zones based upon somewhat different factors.

The site is located within Zone B2. Zone B2 encompasses areas of high noise, but is subject to less risk. The projected 65 decibel noise contour forms the basis for the zone boundary. The actual boundary follows roads, parcel lines or other geographic features that lie generally just beyond the contour line. Lands within the APZs are excluded from Zone B2. Most of the zone lies adjacent to the runway.

Lying just beyond the Air Force defined **Accident Potential Zone (APZs)**, the areas within this zone are subject to sufficient risk to warrant restrictions on the intensity of nonresidential development. Specifically, nonresidential uses would be limited to maximums of 100 people per acre average over a site and 250 people in any single acre. These limits are designed to preclude intensive uses such as major shopping centers and large restaurants. Light industrial uses and office buildings up to three stories are typically consistent with the criteria.

The Specific Plan Area's location adjacent to the runway area subjects the site to a number of unique conditions which are reflected in the project's development regulations and site design, including:

- Height limitations in the 7:1 surface and a building restriction line for structures of over 35 feet;
- Security fencing along the runway property and a clear area beyond;
- Limitations on standing water; and
- Limitations on reflective surfaces facing the runway.

## 2.2 CIRCULATION PLAN

### 2.2.1 Regional

Regional access to the Specific Plan area is provided by Interstate 215 Freeway (I-215). I-215 runs north/south and is immediately to the west of the Specific Plan area. The nearest freeway ramps are approximately  $\frac{1}{4}$  mile north of the Specific Plan area at Van Buren Boulevard. Interstate 215 is currently constructed with three lanes in each direction. Ultimate buildout for Interstate 215 is 10 lanes; 4 mixed-flow lanes and one high occupancy vehicle lane in each direction. The nearest on-ramps are present at Van Buren Boulevard. Van Buren Boulevard also provides regional connections to State Route 91 and 60 Freeways.

Figure 2-5, *Circulation Plan*, identifies the planned roadway system serving the Specific Plan area.

### 2.2.2 Arterial Highways

#### *Van Buren Boulevard Extension*

Van Buren Boulevard is a primary east/west corridor. It provides access to I-215, and is currently designated as an Augmented Urban Arterial to the west of I-215.

On the east side of I-215, Van Buren Boulevard is currently planned as a Major Arterial with two northbound lanes, two southbound lanes and a raised landscaped median. It is constructed consistent with this standard for approximately 1,400 feet along the frontage of the March Field Air Museum, although the street's median is painted rather than raised. Van Buren Boulevard currently terminates approximately 300 feet to the north of the Specific Plan area.

Project-related improvements would include the extension of Van Buren between its existing terminus and the project site, constructed as a divided Modified Secondary Highway, with two northbound and two southbound lanes with a center turn median design, and an on-street bicycle lane. The Modified Secondary Highway street section would include a 97-foot right-of-way (see Figure 2-3, *Van Buren Extension Typical Cross Section*) along the frontage with I-215 and along the site's southern edge, connecting with Western Way. The proposed Van Buren extension improvements would occur within the property boundaries and would be dedicated to the JPA. The Van Buren Boulevard extension by definition (JPA General Plan) is a designated truck route.

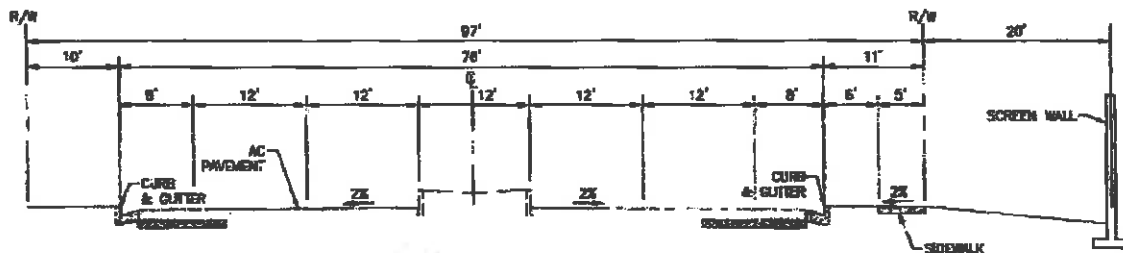


Figure 2-3 Van Buren Extension Typical Cross Section

### Western Way Extension

Secondary access to the Specific Plan area would be provided through the construction of a new roadway extending south from the site's southeastern border to Nandina Avenue, as an extension of existing Western Way.

The Western Way extension would **ultimately** be constructed as a Secondary Arterial as defined in the Perris Commerce Center Specific Plan, with an **ultimate** right-of-way of approximately 94 feet (see Figure 2-4, *Western Way Typical Cross Section*). This road would be an off-site infrastructure improvement encompassing approximately three acres. Western Way is a secondary arterial in the City of Perris Commerce Center Specific Plan originally planned to extend to the March JPA boundary. It is a designated truck route.

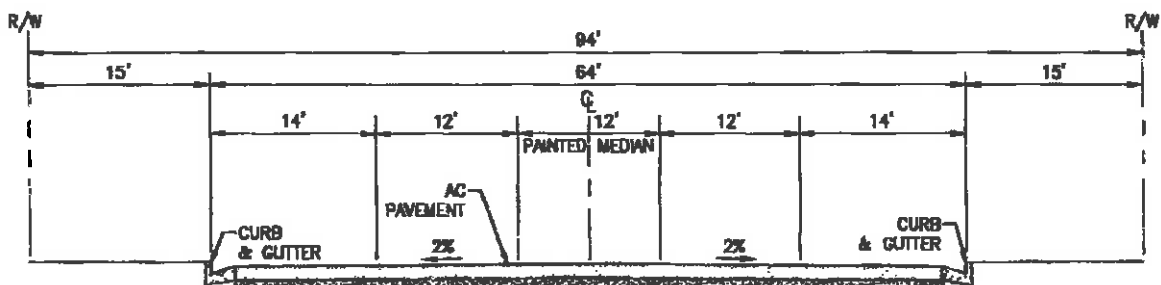


Figure 2-4 Western Way Typical Cross Section **(Ultimate)**

### 2.2.3 Site Access

The Specific Plan incorporates **six-multiple** driveways to provide direct access from the planned Van Buren Avenue and Western Way extensions. Site access may be controlled by security gates within the interior of the site, subject to review by the Fire Department for stacking and emergency access. Additional site access points may be provided subject to plot plan review for conformance with JPA requirements limiting driveways to one for every 300 feet of frontage.

### 2.2.4 Parking

Parking within the Specific Plan area will be in several locations. Employee and visitor parking will be provided in parking areas separated from the truck docks and service areas as shown on the Conceptual Site Plan, Figure 2-2. These parking areas will be landscaped, with parking lot trees shading parking spaces. Truck docks and trailer parking stalls **are-may be** located on the front and rear of each building. Truck docks and parking on the front of the project facing Van Buren Boulevard will be screened with a screen wall. Bicycle parking will be provided. Parking standards for the Specific Plan area are outlined in the Development Regulations section of this document (See Section 3, *Development Regulations*).

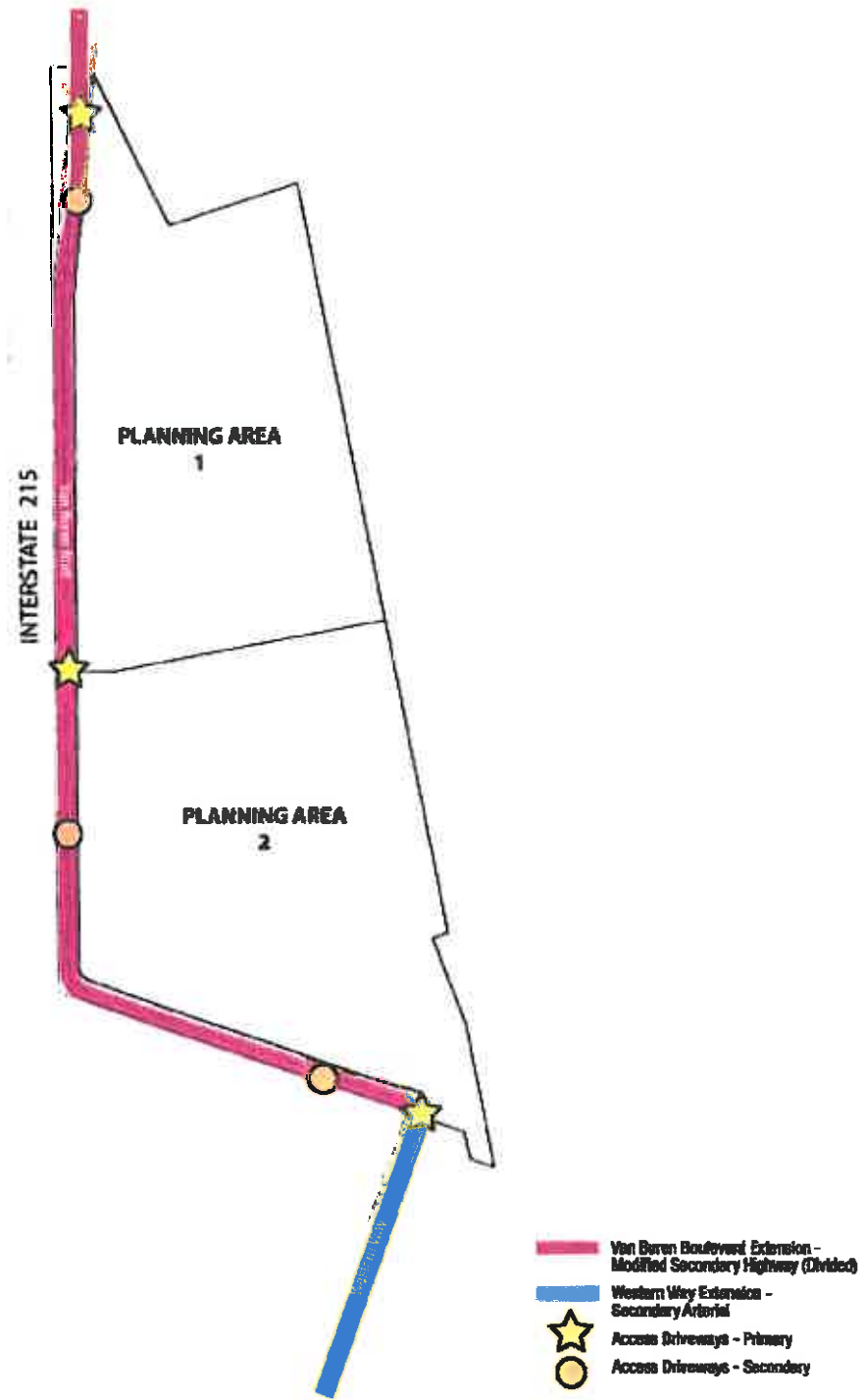


Figure 2-5 Circulation Plan

## 2.3 INFRASTRUCTURE AND SERVICES

The Veterans Industrial Park 215 Specific Plan will require a variety of public facilities and services to support and serve the needs of its businesses. The infrastructure system will seek to incorporate the highest level of sustainability achievable for a project of its kind and in its specific geographic location.

The various public facilities will be designed to enhance and complement the vision and design objectives of the Project and all facilities will be developed to meet or exceed the required industry standards of the respective service providers and as required by the applicable government standards

Services include: water, sewer, storm drainage, solid waste disposal, fire and police protection services. Table 2-2, *Service Providers*, lists the various service providers for the Project.

Service	Provider
Water	Western Municipal Water District (potable) Eastern Municipal Water District (Fire Flow)
Wastewater	Western Municipal Water District
Drainage	Riverside County Flood Control District
Electric Service	Southern California Edison
Gas Service	Southern California Gas Company
Communications	Frontier/Spectrum
Fire Protection	Riverside County Fire Department
Police Protection	Riverside County Sheriff
Solid Waste Disposal/Recycling	Waste Management Inland Empire

### 2.3.1 Water Service

The Specific Plan Area is located within the Riverside retail service area of the Western Municipal Water District (WMWD). WMWD is a member agency of the Metropolitan Water District (MWD), purchasing water from MWD and providing wholesale and retail water within its district boundaries. Water sources from WMWD primarily depend on imported water resources. WMWD purchases both Colorado River and State Water Project water from Metropolitan Water District of Southern California (MWD). Fire water will be provided by Eastern Municipal Water District (EMWD) through an Inter-Agency Agreement between EMWD and WMWD. A Plan of Services will be prepared to identify construction of new facilities and required easements.

#### *Existing Facilities*

Existing EMWD 8-inch water lines are present in Nandina Avenue and Western Way to the south of the Specific Plan Area in the City of Perris. According to EMWD, fire flow supply is available from EMWD's 1705 pressure zone. In addition, an existing 12-inch WMWD water line is present near the northern edge of the Specific Plan Area, within the March Air Museum property.

MWD maintains a currently unused 97-inch transmission main in an easement that follows the proposed alignment of the Western Way extension and the Van Buren Boulevard extension. This line will be protected in place.

The U. S. Air Force maintains an existing 354-foot deep groundwater monitoring well, located within the property near the northern boundary with the **March Airfield** Museum. This well will be protected in place within the truck loading area on the runway side of Building 1/Planning Area 1. An easement ~~will-would~~ be recorded providing for access to the well. ~~Alternatively, the Air Force may opt to relocate the well at their discretion.~~

#### ***Proposed Facilities***

***Domestic Water.*** The proposed domestic water would be provided by WMWD. Water for the project would be extended from the existing 12-inch WMWD water line within the March Air Museum property to Van Buren Boulevard then southwards along the extension of Van Buren Boulevard. Water laterals would extend from the 12-inch lines to service the buildings.

***Water for Fire Suppression.*** Water for fire suppression systems will be provided to the buildings through extension of an 18-inch EMWD line located in Western Way. The 18-inch fire service line will extend northwards within the Van Buren extension right-of-way. A new ~~1210~~-inch fire water line loop will be provided within the Specific Plan area to each building to provide for on-site fire water supply. Fire flow of 4,000 gpm at 20 PSI is required by the Fire Department.

Due to the size of the proposed industrial/logistics uses, a Water Supply Assessment ~~will be was required, required and has been prepared and adopted provided~~ by WMWD.

Figure 2-6a, *Conceptual Water Plan*, illustrates the existing and proposed water infrastructure to serve the site. Changes in water and sewer line size and alignment may occur as part of final engineering.

The Project would be required to plan and install water-efficient devices and landscaping in accordance with applicable ordinances, including use of drought tolerant species appropriate to the climate and region. Although the Project is not located near any existing recycled water distribution infrastructure, should such facilities be constructed in the future, the Project would be required to use non-potable recycled water for irrigation to the extent permitted by law.

### **2.3.2 Wastewater Service**

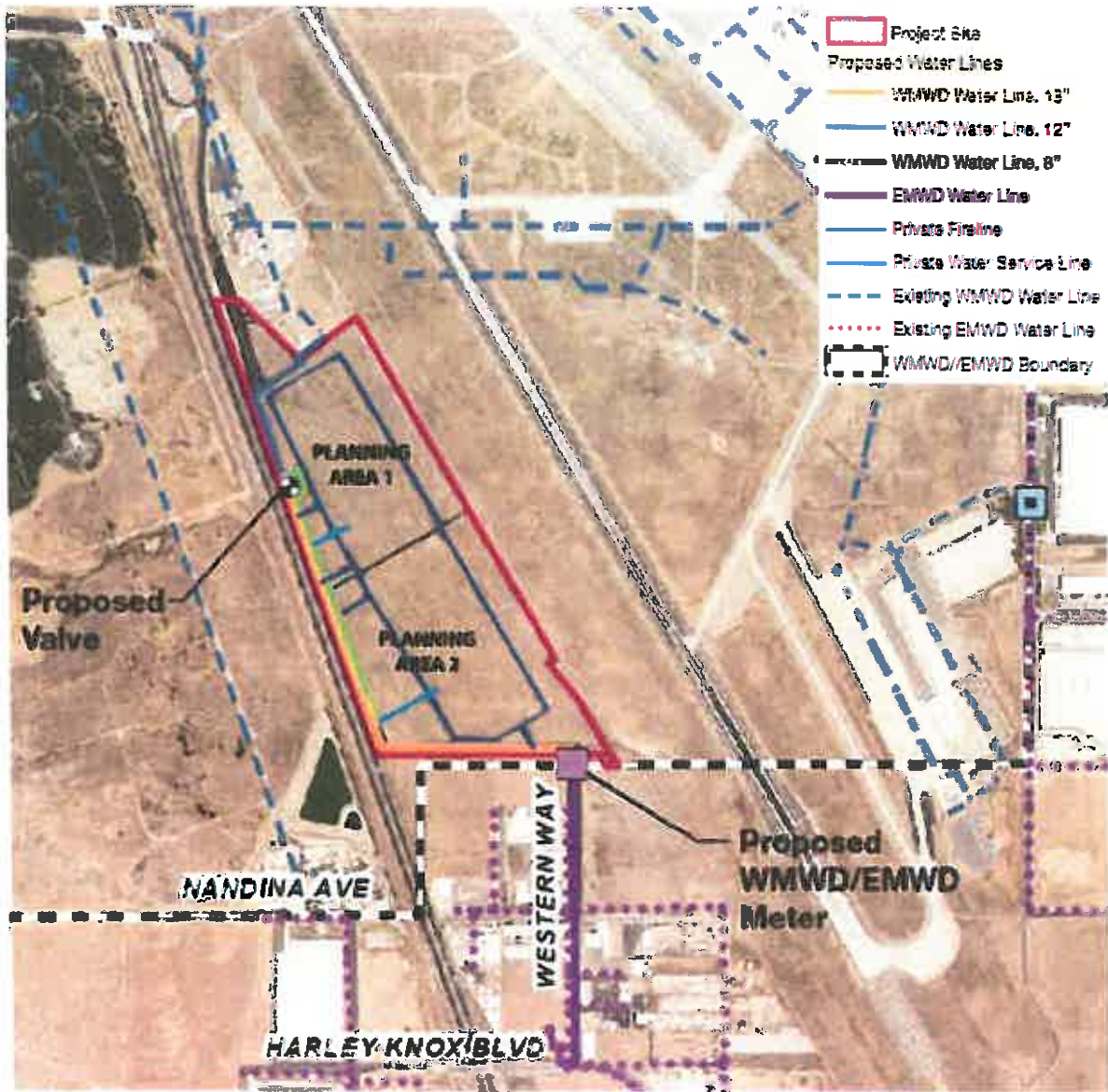
Sewer service in the Specific Plan area is provided by Western Municipal Water District (WMWD).

#### ***Existing Facilities***

Existing 10- and 12-inch sewer lines are present to the south of the project in Nandina Avenue in the City of Perris, however these lines are within EMWD and will not serve the project. In addition, a 10-inch WMWD sewer force main crosses the site's southern edge, continuing west across the I-215 freeway.

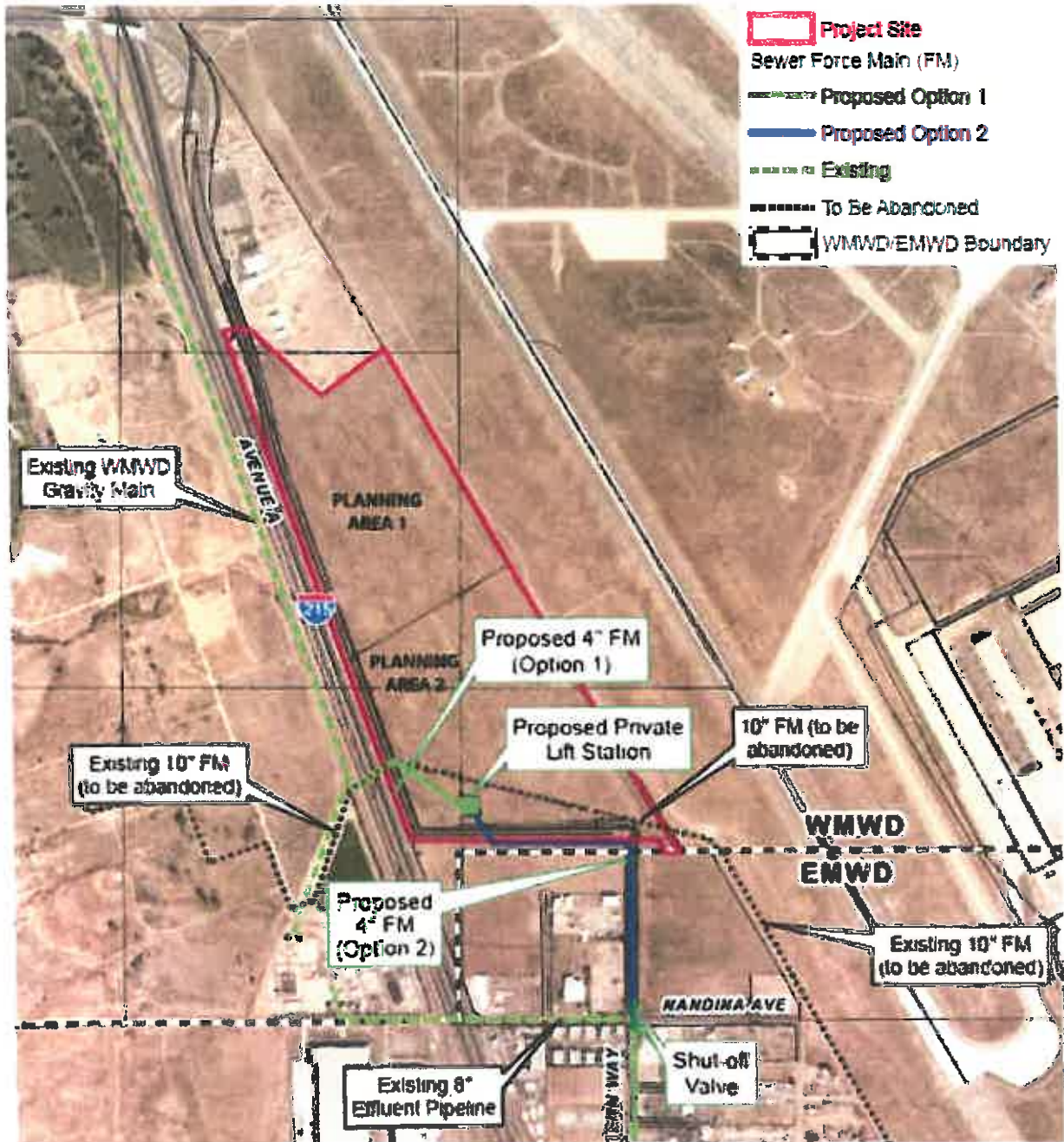
#### ***Proposed Facilities***

The proposed project would relocate the existing WMWD sewer force main within the proposed alignment of Van Buren Boulevard along the Specific Plan area's southern edge. Wastewater from the Specific Plan area would be conveyed via on-site private sewers to a proposed sewer sump and pump system, connect to a new force main, and then connect with a relocated force main. Figure 2-6b, *Conceptual Sewer Plan*, illustrates the existing and proposed wastewater infrastructure to serve the site. Changes in water and sewer line size and alignment may occur as part of final engineering.



Source: Webb & Associates

Figure 2-6a Conceptual Water Plan



Source: Webb & Associates

Figure 2-6b Conceptual Sewer Plan



### 2.3.3 Storm Water Management

The project site slopes gently from northwest-to-southeast, with elevations ranging from approximately 1,525 feet to 1,500 feet above mean sea level. An existing earthen channel runs from the site's northwest corner to its southeast corner, and a second drainage course is present which runs in an east-west direction, connecting with the earthen channel. This channel collects off-site storm water from culverts beneath I-215 and conveys it south through the airport property. This earthen channel conveys runoff southerly towards Heacock Street and discharges into Perris Valley Channel in the City of Perris, ultimately discharging to the San Jacinto River, Canyon Lake, and Lake Elsinore. The Specific Plan Area is located in the Riverside County Flood Control District's Perris Valley Area Drainage Plan.

#### *Off-Site Drainage*

Off-site drainage from upstream properties to the west of the Specific Plan Area is discharged onto the site and into the existing earthen channels from four Caltrans culverts under I-215. To develop the site, this off-site water will be conveyed directly to a proposed ~~concrete-lined trapezoidal channel~~ 14' x 7' to 10' x 10' reinforced concrete box storm drain system on the Specific Plan Area's ~~eastern edge~~ western edge adjacent to ~~the runway~~ Van Buren Boulevard, ultimately discharging to the existing earthen drainage swale at the site's southeastern corner to an interim off-site outlet (see Figure 2-7, Drainage Plan). This off-site water will not be comingled with untreated on-site flows.

#### *Project Drainage*

The site was divided into two parcels/drainage areas with stand-alone drainage facilities, consistent with the two Specific Plan Planning Areas. As noted above, the existing earthen channel will be realigned to the property's ~~eastern-western edge~~ along the east and north sides of Van Buren, and a reinforced concrete box storm drain system will be constructed to collect and convey the off-site flows around the subject property, ~~a concrete trapezoidal channel constructed~~. On-site storm water will be collected, either by surface flow or storm drains, and directed to ~~two-three~~ bio-retention/detention basins. Each basin is sized to have storage capacity ~~above-for~~ the water quality treatment volume as well as to detain and mitigate higher storm events. ~~A pump system will be used to convey water from the basins to the proposed trapezoidal channel since the bottom of the basin is lower than the proposed channel. These pumps will regulate the rate of discharge to mimic pre-development flow rates. Water from the basins will be conveyed to an on-site overflow drain which will convey the runoff to the south and ultimately connect to the new reinforced concrete box storm drain along the south side of the project, north of Van Buren Avenue.~~

All drainage facilities ~~are-will~~ be sized to collect and convey for the 100-year storm event flows. All observable water in both basins will be ~~pumped-discharged out within 24 hours of the end of a storm event. This is a betterment from MIPA's standard criteria of within 48 hours after the end of a storm event.~~ 48 hours after the end of a storm event. Figure 2-87, *Drainage Plan*, illustrates the drainage concept for the project.

#### *Water Quality*

The site is subject to water quality requirements of March Joint Powers Authority (MIPA) and complies with the 2010 Santa Ana MS4 permit. These require that Low Impact Development (LID) Best Management practices (BMPs) are limited to *Infiltration* or *Harvest* and/or bio-retention unless proven infeasible. A WQMP was prepared for the project by Huitt-Zollars (Revised December 2019). The WQMP concludes that both Infiltration and Harvest and Reuse are Infeasible and other treatment control BMPs must be considered. Infiltration was deemed infeasible because the Geotechnical Engineer has

determined that the project site soils have no infiltration capacity and recommended that infiltration BMPs should not be used. The soil is impermeable and infiltration rates have been determined to be less than 1.6 inches/hour. Harvest and Use is not utilized because the anticipated demands for irrigation and toilet use are less than their respective required amounts. Therefore, bio-retention BMPs are considered for this site.

Thus, the primary BMPs to be implemented will be construction of ~~two~~ three bio-retention/bio-treatment basins (refer to Figure 2-7, *Drainage Plan*). Catch basin filters will be provided in all on-site catch basins as pre-treatment control prior to water being conveyed to the basins. The water collected into each basin is then discharged to an on-site storm drain pipe which will convey the runoff to the south and ultimately connect to the new reinforced concrete box storm drain along the south side of the project, north of Van Buren Avenue. The runoff collected in the Reinforced Concrete Box storm drain system will then be conveyed to the east and discharge to the existing channel on March Air Reserve Base Property which ultimately flows to the south end of the Base and then to the Perris Valley Channel. ~~The water collected into each basin is then pumped to a storm drain channel to the East and ultimately conveyed to the existing earthen channel which flows towards the Perris Valley Channel.~~

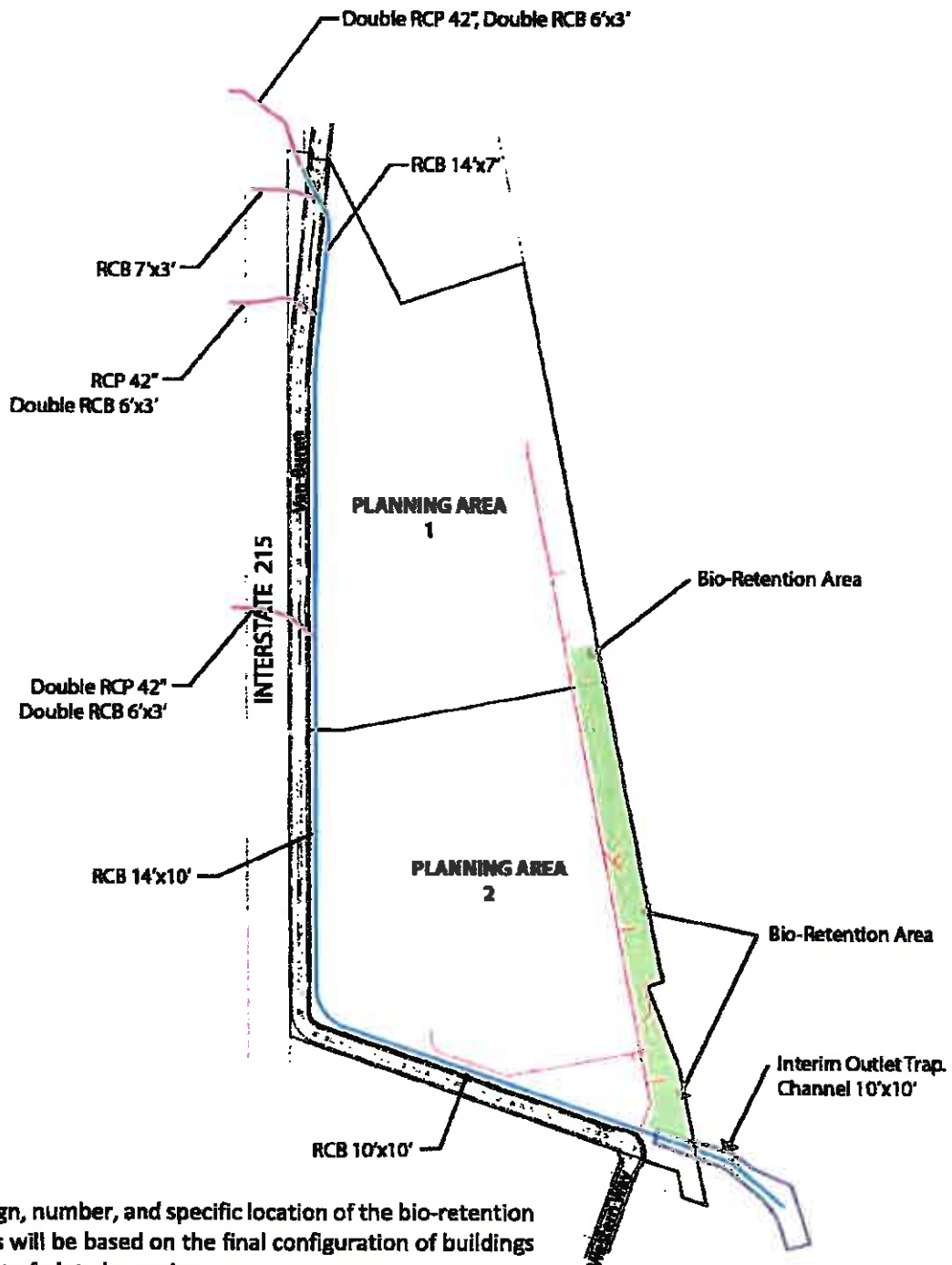
The site is located within an area identified by the Riverside County Flood Control District as being within an area susceptible to streambed erosion (aka Hydrologic Conditions of Concern, or HCOC). Even though the LID design (bio-retention basins) adequately addresses the water quality requirements for the project, this design could still create streambed erosion. ~~The~~ the project site is subject to hydromodification and thus source-control BMPs must also be used for the project to reduce flows to reduce the potential for erosion. The on-site bio-retention basins will also serve as detention basins to mitigate post-development storm water runoff rates down to levels equivalent to the pre-developed condition, thus addressing the HCOC.

Additionally, source control BMPs will be used within the project, including permanent structural features with ongoing operations and maintenance. Some of the source control BMPs used include, "Only Rain Down the Storm Drain" stenciling on catch basins, litter control at truck docks, underground fire protection service and fire sprinkler tests, storm drain filters, landscape and irrigation, and sweeping in plaza/parking areas.

*Hydromodification refers to changes in runoff characteristics caused by altered land use and increase of impervious areas.*

*Too much hydromodification can cause erosion of stream banks and beds. BMPs can include structural BMPs to reduce flows or volumes thus reducing impacts to downstream channels.*





Source: Webb Associates/Hultt-Zollars

Figure 2-7 Drainage Plan

### 2.3.4 Grading

The grading plan for the site creates building pads ~~for two buildings, parking area and, two bio-retention basins, and a trapezoidal channel on the site's eastern edge.~~ The grading plan includes approximately ~~562,116 cubic yards of cut, and 433,974 cubic yards of fill, inclusive of remedial grading (over excavation).~~

Figure 2-8, *Conceptual Grading Plan*, illustrates the conceptual grading for the site. Final grading design and quantities will be based on final engineering ~~and a final plot plan.~~ Any import or export of soil will be detailed on the Tentative Parcel Map for the project and in the final grading plans.

### 2.3.5 Dry Utilities

#### *Natural Gas Service*

The Southern California Gas Company provides natural gas service to the Specific Plan Area.

#### *Electrical Service*

Southern California Edison provides electrical service to the Specific Plan Area. Electrical service lines are present adjacent to the site to the north, associated with the March Air Museum. Electrical service will be extended to the Specific Plan Area along the Van Buren Boulevard extension to the proposed buildings.

### 2.3.6 Solid Waste

Solid waste generated on the Specific Plan area is currently collected by Waste Management Inc. (WMI).

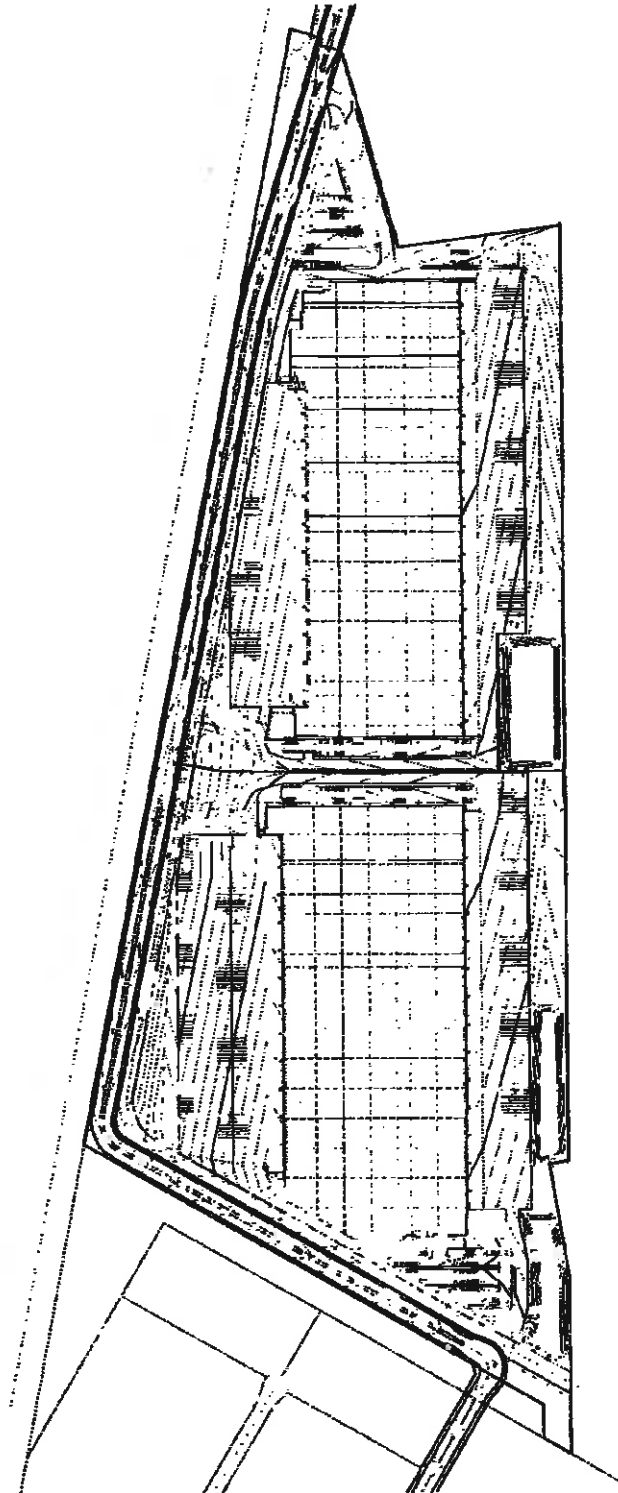
Solid waste in western Riverside County is disposed of at the El Sobrante, Lambs Canyon, and Badlands landfills. The majority of waste from the Specific Plan Area would be sent to the Badlands Landfill, located at 31125 Ironwood Avenue in Moreno Valley, with residual waste sent to the Lamb Canyon Landfill, located at 16411 Lamb Canyon Road in the unincorporated County of Riverside. Both landfills are owned and operated by Riverside County.

In order to reduce the amount of material generated by the Specific Plan's planned future development to meet the State's mandate of 50% solid waste diversion, the Specific Plan will comply with the requirements of the County of Riverside's Source Reduction and Recycling Element (SRRE) and the provisions of AB 341, which focuses on increased commercial waste recycling. Typical of large logistics uses, the logistics buildings will incorporate trash compacting areas.

### 2.3.7 Police and Fire Service

Law enforcement services in the March JPA planning area fall under the Riverside County Sheriff's Department. Sheriff substations are located within the cities of Moreno Valley, Riverside, and Perris.

Fire Services fall under the jurisdiction of the Riverside County Fire Department. Fire protection services are provided by existing County fire stations in Moreno Valley and non-County fire stations from the March Air Reserve Base and neighboring City of Riverside through mutual aid agreements. Existing County Station 6, located at 22250 Eucalyptus Avenue in Moreno Valley would provide fire response to the Specific Plan Area. Station 6 is located approximately six miles from the Specific Plan Area.



Source: Huitt-Zollars

Figure 2-8 Conceptual Grading Plan

# I

## 3 DEVELOPMENT REGULATIONS

This chapter discusses the general provisions and specific development standards for uses within the Specific Plan area, including setbacks, height, and parking requirements.

### 3.1 GENERAL PROVISIONS

The California Government Code (Title 7, Division 1, Chapter 3, Article 8, Sections 65450 et seq.) grants authority to cities and agencies to utilize Specific Plans for purposes of implementing the goals and policies of the agency's General Plan.

This Specific Plan establishes a set of regulations, standards, guidelines, and processes for the proposed development, and shall constitute the zoning for development within the Specific Plan area.

This section has been prepared in accordance with California Government Code Section 65450, et seq. and the March JPA Development Code (Section 9.13). Regulations are included for the proposed logistics land use.

Application of these regulations is specifically intended to provide the most appropriate use of the land, create a harmonious relationship among land uses and protect the health, safety and welfare of the community.

The following General Development Standards apply to all uses within the Specific Plan Area.

#### 3.1.1 Applicability

The Veterans Industrial Park 215 Specific Plan has been developed as both a regulatory and a land use policy document, which, upon adoption by ordinance will constitute the zoning for the property. Development plans or agreements, tract or parcel maps, plot plans or any other action requiring ministerial or discretionary approval of the subject property must be consistent with the Specific Plan. California Government Code, Section 65454 requires that a Specific Plan be consistent with the General Plan. Upon adoption, actions deemed to be consistent with the Veterans Industrial Park 215 Specific Plan shall be judged to be consistent with the March JPA General Plan.

Where conflicts exist between the standards contained in this Specific Plan and those found in the March JPA General Plan or Development Code, the regulations and standards in this Specific Plan shall take precedence. Any area of site development, administration, review procedures, environmental review, landscaping requirements, and regulations not expressly addressed by this Specific Plan document shall be subject to the provisions of the March JPA Development Code, using the context and objectives of this Specific Plan as a guide.

#### 3.1.2 Severability

In the event that any regulation, condition, program, portion or policy of this Specific Plan or the application thereof to any person or circumstance is held to be invalid or unconstitutional by any court of competent jurisdiction, such portions shall be deemed separate, distinct and independent provisions and shall not affect the validity of the remaining provisions of this Specific Plan or applications thereof which can be implemented without the invalid provision or application.

#### 3.1.3 Consistency with Specific Plan

Properties within the Veterans Industrial Park 215 Specific Plan shall be developed in general conformance with the Land Use Plan (Figure 2-1, *Land Use Plan*). Development of properties governed by the Specific Plan shall be in accordance with the mandatory requirements of all March JPA ordinances (unless specifically revised herein) and state laws, and shall conform substantially to the Veterans Industrial Park 215 Specific Plan, as filed in the office of the March JPA Development Services Department, unless otherwise amended.

Except for the Specific Plan Development Standards/Design Guidelines and Substantial Conformance procedures adopted with the Veterans Industrial Park 215 Specific Plan, no portion of the Specific Plan which purports or proposes to change, waive, or modify any ordinance or other legal requirement for development shall be considered to be part of the adopted Specific Plan.

### 3.1.4 Subdivision Map Act

Lots created pursuant to the Veterans Industrial Park 215 Specific Plan and the concurrently processed tentative parcel map, shall be in conformance with the development standards of the zoning applied to the property and all other applicable JPA standards, as well as the Subdivision Map Act.

### 3.1.5 Determination of Unlisted Use

Any land use proposal not specifically covered by the provisions contained herein shall be subject to determination by the Community Development Director in accordance with Section 9.01.060 of the March JPA Development Code.

### 3.1.6 Interpretation

The development standards and regulations contained in this Specific Plan shall supersede the standards contained in the March JPA Development Code, except where specifically provided in this Specific Plan. Whenever the provisions contained in this Specific Plan conflict with the Development Code, the provisions of this Specific Plan shall take precedence. Any ambiguity concerning the content or application of the Specific Plan shall be resolved by the JPA's Planning Director or their designee in accordance with Development Code Section 9.01.060. Such interpretations shall take into account the stated goals and intent of this Specific Plan. If requested or appealed, the Commission may review any administrative interpretation.

### 3.1.7 Definitions

Unless otherwise specified below, terms used in this document shall have the same definitions provided in the JPA's Development Code. If a word is not defined in this section or in any provision of the Development Code, the Planning Director shall determine the correct definition.

- **E-Commerce.** E-Commerce is the buying and selling of goods and services over an electronic network, primarily the internet. This use includes internet fulfillment centers, in which orders are received from affiliated stores or other locations, processed, and filled. The number of employees, and therefore amount of employee parking, is higher than a high cube distribution center.
- **Mezzanine.** As defined herein, a mezzanine is an elevated, occupied floor above the ground floor of a larger industrial space used for office or other enclosed work space. For purposes of this Specific Plan, mechanical mezzanine platforms (typically relocatable, freestanding steel structures) used for logistics/high cube purposes are not counted in the square footage allocated to mezzanines in the land use table nor counted towards the maximum Floor Area Ratio (FAR) or parking counts.
- **High Cube Warehouse/Distribution Center.** High-cube warehouses or distribution centers are primarily for the storage and/or consolidation of manufactured goods (and to a lesser extent, raw materials) prior to their distribution to retail locations or other warehouses but may also accommodate minor assembly as an ancillary use. These facilities are generally very large buildings characterized by a small employment count due to a high level of automation/mechanization, and truck activities are frequently outside of the peak hours of the adjacent street system.
- **Warehousing, with Distribution** means the use of a building or buildings primarily for the interim (short-term) internal storage of goods of any type, which may include sales of goods (e.g. retail sales



or wholesaling). This use is generally engaged in receipt and distribution of goods, products, supplies, etc., with incidental storage and is typically identified with a quick turnaround of such goods.

### 3.1.8 Design Guidelines

Development shall be designed and built in substantial conformance with the Design Guidelines contained in this document (Refer to Section 4, *Design Guidelines*).

### 3.1.9 March Air Reserve Base Performance Standards

The Veterans Industrial Park is located in MARB Airport Influence Zone, therefore, all development within the plan shall comply with the following measures:

- **Avigation Easements.** Prior to recordation of a final map, issuance of building permits, or conveyance to an entity exempt from the Subdivision Map Act, whichever occurs first, an avigation easement shall be conveyed to March Air Reserve Base/March Global Port through the March JPA and will provide and disclose a "Notice of Airport in Vicinity" to building tenants.
- **Noise Standard.** All building office areas shall be constructed with appropriate sound mitigation measures as determined by an acoustical engineer or architect to insure appropriate interior sound levels. This standard will be confirmed as part of building permit plan check.
- **Retention and Water Quality Basins.** All retention and water quality basins shall be designed to de-water within 48 hours of a rainfall event.
- **Lighting Plans.** Prior to issuance of building permit, lighting plans shall be submitted to an airport lighting consultant or March Air Reserve Base/March Inland Port (MARB/MIP), for review and comment prior to issuance of building permits. Lighting shall consist of High Pressure Sodium or LED fixtures (below 2500 Kelvin).
- **Height Restrictions per Federal Aviation Regulations Part 77.** The Federal government has developed standards for determining obstructions in navigable airspace. Federal Aviation Regulations Part 77 defines a variety of imaginary surfaces at certain altitudes around airports. The Part 77 surfaces include the primary surface, approach surface, transitional surface, horizontal surface and conical surface. Collectively, the Part 77 surfaces around an airport define a bowl-shaped area with ramps sloping up from each runway end. The Part 77 standards are not absolute height restrictions, but instead identify elevations at which structures may present a potential safety problem. Penetrations of the Part 77 surface generally are reviewed on a case by case basis. The project area is located within the Inner Horizontal Surface (Surface E).

The Inner Horizontal Surface is a plane, oval in shape at a height of 150 feet above the established airfield elevation (i.e. 1,685 feet above MSL at the northern end of the runway and 1,638 feet above MSL at the southern end of the runway) extending 7,500 feet around the centerline of the runway. Therefore, structures would need to exceed an elevation of 1,685 feet MSL at the northern end of the runway and 1,638 feet at the southern end before they encroached into this Part 77 surface for the March Air Reserve Base runways. For the Project, FAA review would be required for any structure with a top of roof exceeding 1,497.5 feet above MSL<sup>1</sup>.

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<sup>1</sup> [Airport Land Use Commission staff analysis](#)

### 3.2 PERMITTED USES

#### 3.2.1 Permitted Uses

- a) Heavy and Light Logistics/distribution and warehousing, including high-cube warehousing (including uses requiring refrigeration of up to 10,000 square feet)
- b) E-Commerce, including fulfillment centers
- c) Research and Development
- d) Light Manufacturing and Assembly including aviation-related manufacturing

#### 3.2.2 Ancillary Uses

An ancillary use is a permitted use which is subordinate to the primary permitted use. The following are uses which are permitted within the Specific Plan area as ancillary uses in support of and subordinate to the primary permitted uses.

- a) Cellular transition facilities and structures.
- b) Offices, including corporate, subsidiary and regional management offices.
- c) Maintenance facilities (internal) associated with a permitted use.
- d) Showrooms and retail uses not to exceed 5% of the gross building area.
- e) Outdoor vehicle, equipment and container storage ancillary to an approved use accommodated within an approved building (outdoor storage shall be screened when facing the public right-of-way).
- f) Short-term construction yards.
- g) Public utility uses and structures.
- h) Employee support uses including cafeteria/café and training facilities as an ancillary use.
- i) Uses determined by the Planning Director to be similar and not more intensive than other allowed ancillary uses.

#### 3.2.3 Conditional Uses subject to further Environmental Review

- a) Aviation Related Facilities
- b) Facilities incorporating greater than 5% total building area in showroom/retail space.
- c) Cold Storage facilities larger than 10,000 S/F incorporating ammonia refrigeration or other refrigerants which are combustible or toxic.

#### 3.2.4 Prohibited Uses

The following uses shall be prohibited within the Specific Plan:

- a) Outdoor new or used car, truck, trailer and equipment sales.
- b) Public Assembly facilities, inclusive of churches, assembly halls, schools, and libraries.
- c) Retail/Restaurant as a primary use.
- d) Above ground Petroleum Storage containers and below ground storage containers in excess of 10,000 gallons.

### 3 | DEVELOPMENT REGULATIONS

- e) Uses inconsistent with the B2 Airport Land Use Compatibility Zone as identified in the current March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan.
- f) Child care.
- g) Medical related facilities involving the treatment of patients.
- h) Solar panels: For projects that consider a roof or ground mounted solar photovoltaic system, a glare/glint study must be submitted to March ARB to examine the potential impacts on flight operations.
- i) Temporary Uses except for construction trailers and security offices.

### 3.3 DEVELOPMENT STANDARDS

No building or portion thereof shall be erected, constructed, converted, established, altered, enlarged, nor shall any legal lot or premises be used unless the legal lot or premises and building comply with the following regulations and standards. The following development standards are minimums unless otherwise stated.

Table 3-1 Development Standards	
Item	Dimension/Standard
Floor Area Ratio (maximum):	0.4 <sup>2</sup>
Front Setback	20 feet
Side Setback	5 feet
Rear Setback (at Property Line)	25 feet minimum
Buildings over 35 feet in height	1,224' feet minimum <sup>3</sup> , subject to compliance with the 7:1 slope.
Building Separation	100 feet
Site Landscaping	10%
Building Height (maximum)	<del>48</del> 55 feet <sup>4</sup>

<sup>2</sup> Floor Area Ratio (FAR) shall be averaged - calculated across the entire Specific Plan Area. FAR shall not include mechanical mezzanines.

<sup>3</sup> Measured from runway centerline.

<sup>4</sup> Inclusive of rooftop equipment.

**3.3.1 Parking**

Parking within the Veterans Industrial Park 215 Specific Plan area shall be provided in accordance with the ratios in Table 3-2, *Parking*:

Table 3-2 Parking	
Item	Parking Ratio
Office/Ancillary Retail	1 space/300 sf
Logistics <sup>5</sup>	
0-50,000 sf	1 space/1,000 sf
50,000 – 200,000 sf	1 space/3,000 sf
200,000 + sf	1 space/ 5,000 sf
Bicycle Parking	1 /20 auto stalls/5%
Carpool Stalls	5% of auto stalls
Electric Vehicle Charging Stations	2.5% of auto stalls or per CAL Green requirements

**3.3.2 General Design Standards**

This section is intended to provide the general development regulations and standards for land uses located within the Specific Plan area. The following standards shall apply:

1. Buildings containing the land uses shall consist of quality architectural features.
2. Architectural elements such as pilasters, columns, canopies, porticos, colonnades, arcades, and other architectural elements may be incorporated.
3. In addition to the architectural elements standards expressed in this subsection, color changes, texture changes, and material changes shall be used.
4. Methods to reduce the likelihood of graffiti, such as creeping vines or other methods shall be incorporated, as appropriate.
5. Building entryways shall be clearly defined and incorporate architectural details.

***Trash, Service, and Delivery Areas***

1. Service areas and loading docks shall be screened from view from adjacent streets.
2. All outdoor storage areas for equipment shall be fully screened from view.
3. When appropriate, a landscape buffer may be provided along service/delivery areas.
4. Trash enclosures shall be a minimum six (6) feet in height and should be architecturally compatible with the main building.

***Mechanical Equipment***

1. Rooftop mechanical equipment shall be securely fastened to the roof and fully screened with architectural elements consistent with the overall design of the primary structure.
2. Exterior ventilating and mechanical equipment shall not disturb neighboring occupants and shall be screened, shielded, and/or buffered from sound from adjacent properties.

<sup>5</sup> Mechanical mezzanines shall not require additional parking.

### 3 | DEVELOPMENT REGULATIONS

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3. All mechanical equipment, including aboveground utility boxes, telephone boxes, back flow preventers, cable boxes, or similar structures shall be fully screened from view from the closest adjacent public street. Screening shall not obstruct required equipment access required by the relevant utility provider.
4. Satellite dishes shall be roof-mounted and screened from view.

#### *Lighting and Security*

1. Site lighting shall be low or high-pressure sodium, maximum 750-watt, full cut-off fixtures, with the maximum light fixture height of 25 feet above finished grade, and a maximum lighting level of 0.5 foot candles at the property line. For LED lighting an equivalent wattage level shall be provided.
2. All freestanding light poles shall be located within landscaped areas. Concrete light pole bases shall be painted to match the primary building color or finished to match parking screening walls and shall not exceed 24' above finished grade.
3. No cameras may be oriented towards the runway and cameras must not record base airfield operations.
4. Perimeter fencing adjacent to airport runway must be a minimum of eight feet in height with three strands of barbed wire.

# 4 DESIGN GUIDELINES

**This chapter explains design concepts and establishes design policies and design guidelines for development within the Specific Plan area. These guidelines address the built form as well as general guidelines related to mobility and parking, landscaping and signage.**

The guidelines within this chapter describe and illustrate building designs, concepts, and features that will promote the high-quality development that is envisioned for the Specific Plan area. The design guidelines should be used in conjunction with the development standards described in Chapter 3: Development Regulations.

These design guidelines will serve to promote cohesive design and community identity. Graphics and photographic images are included as a visual reference and should not be interpreted as the only design solution. Creative approaches are encouraged.

These Design Guidelines serve the following functions:

- To provide the March JPA with assurance that the Veterans Industrial Park will be developed in accordance with the quality and character described within this Specific Plan.
- To establish design guidelines for site design, architecture, circulation, parking, lighting, and other distinguishing features.
- To provide guidance to JPA staff, and the Commission in the review of future implementing projects within the Specific Plan area
- To provide developers, builders, planners, architects, landscape architects and property owners with guidelines and recommendations, to aid in maintaining the high level of community cohesiveness and unity, while still allowing for a degree of personal expression.
- Encourage sustainable design solutions that reduce energy consumption, use water efficiently, and minimize waste.
- Create simple building designs that result in efficient use of space, materials, and resources while maintaining a high level of design integrity and authentic architectural style.

The terms “shall”, “should”, and “may” are used within the Design Guidelines. The term “shall” is used to denote a design standard where compliance is required. The term “should” is used to denote a guideline that is recommended, but not required in all circumstances. The term “may” is used to denote a design treatment that is allowed or optional.

These guidelines may be subject to modification over time to respond to unanticipated conditions, such as changes in the real estate market, specific needs of buildings users, technology advancements, and economic fluctuations.

### 4.1 INTRODUCTION

These Design Guidelines are intended to create quality development while allowing flexibility. Projects implementing this Specific Plan will depict detailed building footprints, parking lot layouts, internal circulation flow patterns, and landscaping, and should be in substantial conformance with the goal of these Design Guidelines. However, the Design Guidelines in this Section are not intended to be interpreted in a way that would unnecessarily burden the Developer(s) and their design professionals with the need to exactly replicate the exhibits included in these guidelines.

These Design Guidelines consist of two principal elements: Architecture and Landscape. These elements define the design concept, physical character, and overall theme of the Veterans Industrial Park. Text descriptions and graphic exhibits are used to convey the overall theme of the project.

The Architectural Design Guidelines address the industrial themed architecture for buildings permitted within the Veterans Industrial Plan and are intended to provide a basis for decisions regarding the

structural environment. A high-quality industrial project is defined by the guidelines provided for architectural design and details, building mass and scale, materials and exterior colors, and articulation.

The Landscape Guidelines present general landscaping requirements, including streetscape design, entry treatments, signage, water quality features, walls and fencing, and lighting. Plant material guidelines provide direction regarding the use of plant materials that complement the overall theme. The Landscape Design Guidelines also provide general requirements relating to water conservation.

### 4.2 ARCHITECTURE GUIDELINES AND STANDARDS

#### 4.2.1 Building Form and Orientation

Building form is a defining feature of architecture. Shape, massing, scale, proportion, and articulation are all components of a building's form. The proposed architecture for the Veterans Industrial Park is a contemporary design appropriate to the proposed industrial use. Building Corners facing Van Buren Boulevard will be utilized for offices and show a higher level of articulation and fenestration than the logistics/warehouse components of the plan.

Figure 4-1, *Example Architecture*, illustrate elevations that comply with these design guidelines. Future building designs may vary from this example. Buildings within the Veterans Industrial Park 215 Specific Plan shall comply with the following guidelines:

- Buildings should be oriented so that loading areas are screened from view from streets and public areas.
- Buildings should be arranged to provide convenient access to entrances and efficient internal circulation for vehicles and pedestrians.
- Visitor parking should be located with convenient access to public building entries
- Indoor or outdoor break areas shall be provided.
- Architectural style should be of a classic, contemporary technical/industrial style with clean efficient lines. Simple geometric forms shall constitute the overall building form. Rectangular forms are encouraged to promote balance and visual interest. Arbitrary, complicated building forms and rooflines should be avoided.
- Building planes visible from Van Buren Boulevard should be articulated using changes in building materials, color, and/or decorative accents/scoring.
- Modulation and variation of building masses between adjacent buildings visible from Van Buren Boulevard or Western Way is encouraged.
- Materials applied to any elevations shall turn the corner of the building to a logical termination point in relation to architectural features or massing.
- Pedestrian entrances to buildings accessible to visitors should be identifiable through changes in massing, color, and/or building materials.
- Primary building entries shall be easily identified through the use of prominent architectural elements, signage, landscaping, lighting, canopies, roof form, hardscape, architectural projections, columns, vertical elements, or other design features that help emphasize the building's entry.







Source: RGA

Figure 4-1 Example Architecture

### **4.2.2 Materials and Colors**

Complementary materials and colors play a key role in developing a pleasing visual environment. Slight variations from building to building are permitted within the Specific Plan area to provide visual interest.

- Materials shall be of a non-reflective material when facing the runway, including exterior ductwork, windows, and roofs.
- Appropriate primary exterior building materials within the Specific Plan area may include tilt-up concrete panels, stucco, and concrete.
- Primary materials may be accented by secondary materials on elevations visible from public streets such as Van Buren Boulevard and Western Way. Appropriate secondary materials may include glass, natural or fabricated stone, metal, and tile or tile panel systems. Highly reflective materials on elevations facing the runway or the aircraft approach path are prohibited.
- Building materials shall be durable and able to withstand long-term exposure to the elements.
- Trim details may include metal finished in a consistent color, plaster, or concrete elements finished consistently with the building treatment. Foam cornice caps or moldings are discouraged.
- Colors and materials for all structures onsite should consist of earth tones. Use of at least two to three different colors, materials or textures is encouraged. Bright, primary colors are discouraged, except in tenant signage logos.
- Large expanses of smooth material (e.g., concrete) shall be broken up with expansion joints, reveals, or changes in texture and color.
- The color of exposed downspouts, service doors and mechanical screens should complement the color of the structure.

### **4.2.3 Windows and Doors**

Windows and doors should be defined by function, consistent in form, pattern, and color. Appropriate treatments consist of functional glass use, a balance of glazing and wall surfaces, with no highly reflective surfaces facing the runway.

- Window layout should be in a repetitive pattern for visual continuity.
- Window and door styles and trims should be consistent within a building and among multiple buildings.
- Mirrored or highly reflective glass is not permitted.
- Pedestrian entries should be clearly defined.

### **4.2.4 Loading Docks and Service Doors**

- Service doors, loading docks, and truck courts should be screened so they are not easily visible from public roads, unless the public road is substantially higher in elevation than the loading areas. Screening may be accomplished with solid walls compatible with the architectural style of the building or by a combination of screen walls, landscaping, and berms. Screen walls may be located at the foot or top of slopes to effectively screen loading areas.
- Docks and truck courts should be separated from visitor and customer parking areas and pedestrian walkways through the use of walls, fences and/or landscaping.

- No loading or unloading activity is permitted to take place from public streets or the internal drive aisles. Trucks shall have clear and convenient access into and within the truck courts of each building and should not disrupt vehicular and pedestrian circulation.

### 4.2.5 Security Elements

#### *Cameras*

The location and appearance of security cameras must be integrated with the architecture. The top of any roof-mounted camera must be below the top of the parapet.

- No cameras may be oriented towards the runway and cameras must not record base airfield operations.
- Cameras may be mounted on poles in parking lots
- Cameras may be mounted on building or screen walls with the top of the camera below the top of the parapet
- The color of the camera housing should match the color of the poles or the building wall.

#### *Inappropriate Treatment*

- Wall-mounted cameras with the top of the camera above the top of the parapet
- Exposed wiring
- Cameras mounted in spheres on arms projecting from building walls.

#### *Fencing*

Along the runway/airport boundary on the Specific Plan area's eastern edge, special security fencing shall be used. Fencing must be a minimum eight feet in height with three strands of barbed wire. This fencing shall be of a durable material (may be chain link) subject to JPA and March Air Reserve Base review.

#### *Gating*

Pedestrian and vehicular access gates visible from public areas (i.e., parking lots, streets, sidewalks, etc.) shall be constructed of a durable material, such as tubular steel.

### 4.2.6 Trash Enclosures

- All outdoor refuse bins or other containers must be screened within a permanent, durable enclosure and oriented away from public roads or other public view.
- The design of trash enclosures must be consistent with the architectural style, color, and materials of adjacent buildings.
- At least one trash enclosure shall be located adjacent to each building. Three sides of the trash enclosures will be constructed of concrete or block walls and the fourth side of a gate.

## 4.3 LANDSCAPE GUIDELINES

### 4.3.1 Landscape Master Plan

Landscape treatments around buildings will be designed to help break up the building massing by incorporating both ~~tall, vertical trees~~ and lower growing and ~~broader canopy trees~~ along Van Buren Boulevard. The ground plane will be landscaped with a mix of shrubs and ground cover plants to create a layered appearance along the western edge of the site adjacent to Van Buren Boulevard. On the eastern

side of the site adjacent to the airfield, landscaping will not include tree plantings and will provide a ground plane with sharp edges between shrubs and groundcover. A Conceptual Landscape Plan for the Specific Plan area is illustrated on Figures 4-3, and 4-4 *Landscape Master Plan*. Trees used within the Specific Plan will be selected from the list on Table 4-1 *Plant Materials*. Shrubs and groundcovers will be selected concurrent with final designs for individual projects within the Specific Plan area and shall be in keeping with the Airport Land Use Commission guidance for landscaping near airports (see Appendix D).

Subsequent landscape plans created by tenants must adhere to the landscape materials outlined in this Section of the Specific Plan.

- Landscaping shall be provided in all setback areas of the Specific Plan area.
- The Specific Plan area shall comply with the landscape design measures to reduce water use contained within the MIPA Development Code Section 9.17 and with the ALUC guidance for landscaping near airports.
- Streetscapes will incorporate low water use plant materials to minimize irrigation needs. Open space areas not planted with living material should utilize permeable materials such as decomposed granite, mulch and/or rocks/cobble to reduce irrigation demands where possible.
- The community shall be irrigated with reclaimed or recycled water if available.
- Planting areas will be irrigated with a high efficiency automatic irrigation system.



Figure 4-2 Van Buren Boulevard Streetscape Edge

### 4.3.2 Water Quality

~~Bio-retentionStormwater~~ basins are included in the drainage plan for the Specific Plan. The basins are shallow impoundments designed to collect, treat, and detain stormwater runoff before discharging it. ~~In addition to functioning as storm-water/water treatment facilities, water quality features such as bioswales need to consider their impact on the overall aesthetics of the project. Whenever possible, these facilities will be carefully sited and integrated into landscaped areas if possible, and to avoid the look of engineered, utilitarian facilities. The design should integrated into the landscape and appear as a landscape feature, while incorporating acceptable plant materialutilize hardscape~~



Infiltration Basin (Source: SoCal LID Manual)

~~materials (i.e. non-vegetated) to avoid attracting birds, in accordance with the ALUC guidance for landscaping near airports.~~

~~Vegetative cover is important to minimize erosion and ensure that treatment occurs in biofiltration basins. To prevent basins from being used as walkways or passive recreation areas to the extent that their primary function is compromised, bioretention facilities will be planted with a combination of small trees along the upper edges, densely planted shrubs, and natural grasses (see Table 4-2).~~

~~Final design of on-site basin landscaping plant materials and will be reviewed by a BASH-certified biologist for compliance with FAA regulations.~~

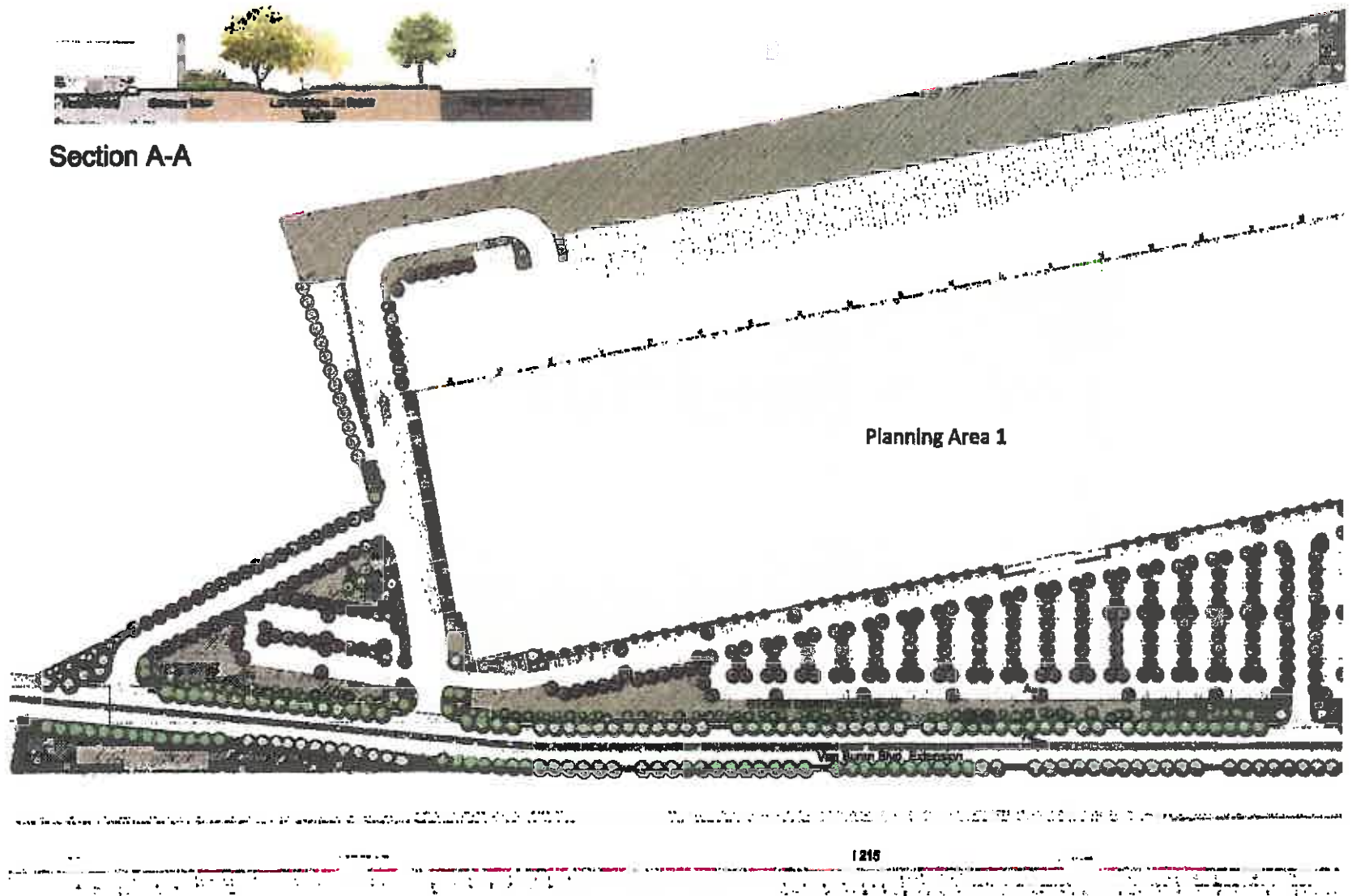
The following guidelines should be followed:

- The basin area should be designed for at least 70 percent mature coverage to maximize biofiltration. Basin bottoms shall be constructed of porous material to allow for water penetration.
- Grasses should be native or ornamental; preferably ones that do not need to be mowed.
- The application of fertilizers and pesticides should be minimal.
- To maintain oxygen levels for the vegetation and promote biodegradation, and to comply with aviation-related restrictions related to ponding, it is important that vegetation not be completely submerged for any extended period of time.

Table 4-1 Plant Materials - Trees				
Botanical Name	Common Name	Size	WUCOLS	Remarks
<i>Cercidium 'Desert Museum'</i>	Blue Palo Verde	24" Box	L	Multi
<i>Cercis occidentalis</i>	Western Redbud	24" box	M	
<i>Chilopsis linearis</i>	Desert Willow	24" Box	L	Multi
<i>Chitilpa tashkentensis</i>	Chitalpa	24" Box	L	Standard
<i>Prosopis ssp</i>	Mesquite Tree	24" Box	M	Standard
<i>Acacia smallii</i>	Acacia	24" Box	L	Multi
<i>Acacia stenophylla</i>	Shoestring Acacia	24" Box	L	Multi
<i>Lagerstroemia indica</i>	Crape Myrtle	24" Box	M	
<i>Pinus canariensis</i>	Canary Island Pine	24" Box	M	Standard
<i>Pinus eldarica</i>	Afghan Pine	24" Box	L	Standard
<i>Rhus lancea</i>	African Sumac	24" Box	L	Standard
<i>Tristania conferta</i>	Brisbane Box	24" Box	M	Standard
<i>Washington robusta</i>	Mexican Fan Palm	10' ht	L	Skinned
<i>Brahia armata</i>	Blue Fan Palm	15-Gal	M	Standard
Table 4-2 Plant Materials - Shrubs				
Botanical Name	Common Name	Size	WUCOLS	Remarks
<i>Cistus 'Sunset Pink'</i>	Sunset Pink Rockrose	5 Gal	M	
<i>Callistemon "Little John"</i>	Dwarf Bottle Brush	5 Gal	M	
<i>Dietes bicolor</i>	Fortnight Lily	5 Gal	M	
<i>Leucophyllum ssp</i>	Texas Ranger	5-Gal	L	
<i>Ligustrum j. Texanum</i>	Texas Privet	5 Gal	M	
<i>Salvia c. 'Allen Chickering'</i>	Allen Chickering Sage	5 Gal	L	
<i>Salvia greggii</i>	Autumn Sage	5 Gal	L	
<i>Salvia leucantha</i>	Mexican Sage	5 Gal	L	
<i>Westrinia f. "Grey Box"</i>	Coast Rosemary	5 Gal	L	
<i>Senna artemisioides</i>	Feathery Cassia	5-Gal	L	

## 4 | DESIGN GUIDELINES

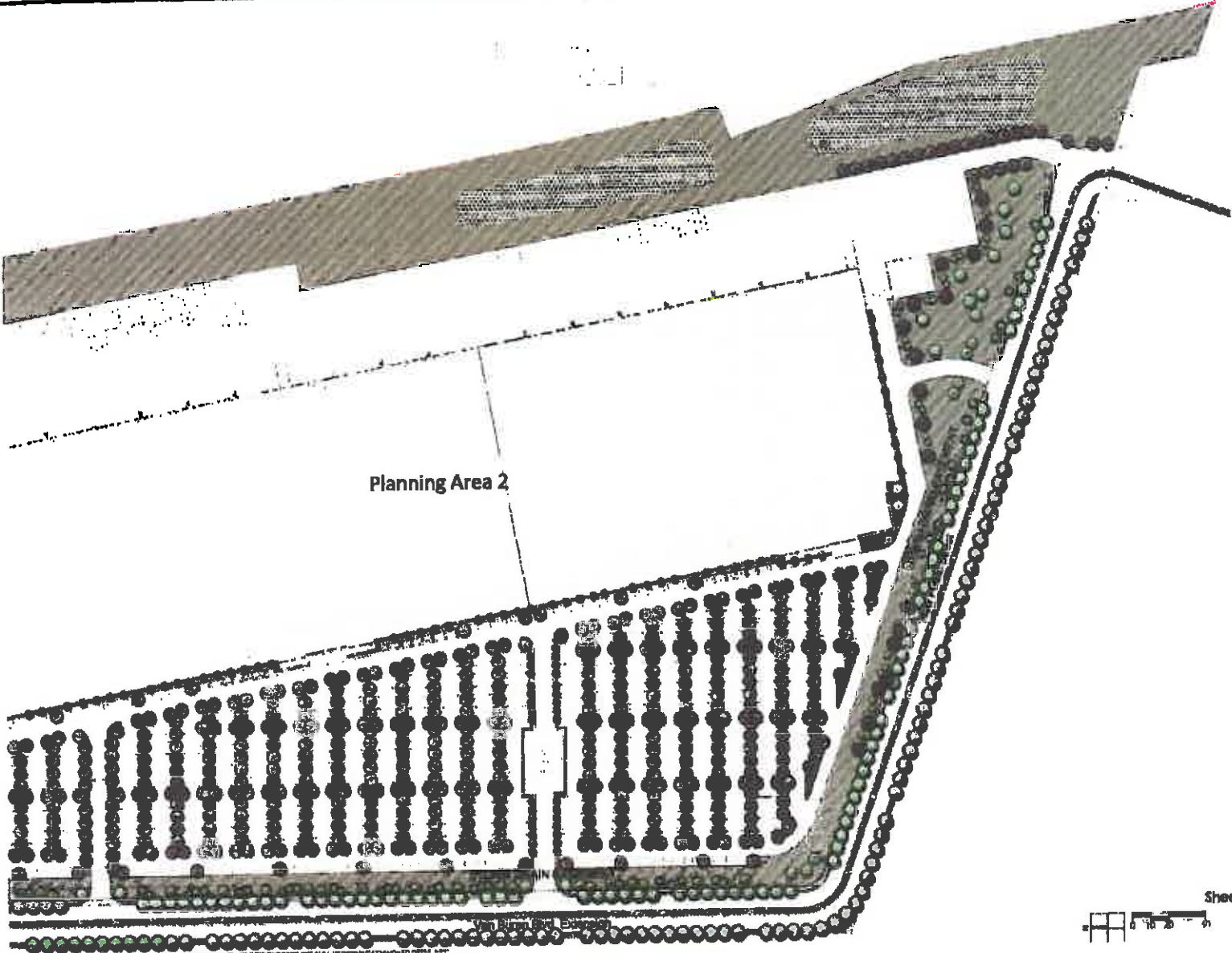
<u>Caesalpinia pulcherrima</u>	<u>Red Bird of Paradise</u>	<u>5 Gal</u>	<u>L</u>		
<u>Tecoma stans</u>	<u>Yellow Bells</u>	<u>5 Gal</u>	<u>L</u>		
<u>Artemisia spp.</u>	<u>Wormweed</u>	<u>5 Gal</u>	<u>L</u>		
<u>Rhamnus spp.</u>	<u>Coffeeberry</u>	<u>5 Gal</u>	<u>L</u>		
<b>Top Rowing Plants</b>					
<u>Latin Name</u>	<u>Common Name</u>	<u>Size</u>	<u>Spacing</u>	<u>MOISTURE</u>	<u>Remarks</u>
<u>Agave spp.</u>	<u>Agave</u>	<u>5 Gal</u>	<u>L</u>		
<u>Aloe spp.</u>	<u>Aloe</u>	<u>5 Gal</u>	<u>L</u>		
<u>Dasylirion wheeleri</u>	<u>Desert Spoon</u>	<u>5 Gal</u>	<u>L</u>		
<u>Hesperaloe parviflora</u>	<u>Red Yucca</u>	<u>5 Gal</u>	<u>L</u>		
<u>Opuntia spp.</u>	<u>Prickly Pear</u>	<u>5 Gal</u>	<u>L</u>		
<u>Echinocactus grusonii</u>	<u>Golden Barrel Cactus</u>	<u>5 Gal</u>	<u>L</u>		
<b>Other Rowing Plants</b>					
<u>Latin Name</u>	<u>Common Name</u>	<u>Size</u>	<u>Spacing</u>	<u>MOISTURE</u>	<u>Remarks</u>
<u>Acacia retolens 'Desert Carpet'</u>	<u>Dwarf Acacia</u>	<u>1 Gal</u>	<u>8" O.C.</u>	<u>L</u>	
<u>Festuca mairei</u>	<u>Atlas Fescue</u>	<u>1 Gal</u>	<u>24" O.C.</u>	<u>M</u>	<u>Grass</u>
<u>Festuca o. 'Glauca'</u>	<u>Blue Fescue</u>	<u>1 Gal</u>	<u>12" O.C.</u>	<u>M</u>	<u>Grass</u>
<u>Hemerocallis hybridus-Yellow</u>	<u>Yellow Day Lily</u>	<u>1 Gal</u>	<u>24" O.C.</u>	<u>M</u>	
<u>Lantana 'Gold Mound'</u>	<u>Yellow Lantana</u>	<u>1 Gal</u>	<u>36" O.C.</u>	<u>L</u>	<u>Non fruiting</u>
<u>Muhlenbergia capillaris</u>	<u>Pink Muhly</u>	<u>1 Gal</u>	<u>36" O.C.</u>	<u>L</u>	<u>Grass</u>
<u>Rosmarinus o. 'Huntington Carpet'</u>	<u>Prostrate Rosemary</u>	<u>1 Gal</u>	<u>48" O.C.</u>	<u>L</u>	



Source: Hunter Landscape

Figure 4-3 Planning Area 1 Conceptual Landscape Plan





Source: Hunter Landscape

Figure 4-4 Planning Area 2 Conceptual Landscape Plan

Latin Name	Common Name
<i>Achillea millefolium</i>	Yarrow
<i>Eschscholzia caespitosa</i>	Foothill Poppy
<i>Juncus bufonius</i>	Toad Rush
<i>Leymus triteoides</i>	Wild Rye
<i>Deschampsia caespitosa</i>	Tufted Hairgrass
<i>Festuca rubra</i> "Moate"	Red Fescue
<i>Hordium brachyantherum</i>	Meadow Barley
<i>Muhlenbergia rigens</i>	Deergrass

### 4.3.3 Utility Placement and Screening

All exterior ground-mounted equipment—including, but not limited to, mechanical equipment, electrical equipment, emergency generators, boilers, storage tanks, risers, electrical conduit, gas lines, cellular telephone facilities, and satellite dishes must be screened from on-site and off-site view by a combination of decorative walls (where appropriate) and dense landscaping.



- Utility boxes should be grouped where possible and placed in landscape setbacks and/or shrub/groundcover areas.
- Above grade utility boxes should be screened and planted to the extent possible while allowing required access and clearance, and providing for adequate sight distance if located near intersections.

*Inappropriate Screening Treatments*

- Screening materials contrasting with adjacent structures
- Chain link fencing
- Lack of landscape buffering

Roof-mounted mechanical equipment shall be fully screened by a parapet wall equal to or exceeding the height of the mechanical units, subject to FAA height limits.

### 4.3.4 Walls and Fences

Walls and fences must be designed as an integral part of the overall architectural or landscaping design concept. Within designated edge treatment areas, proposed fencing shall be included in the required Concept Plan.

- Along the runway/airport boundary on the Specific Plan area’s eastern edge, special security fencing shall be used. Fencing must be a minimum eight feet in height with three strands of barbed wire. This fencing shall be of a durable material (may be chain link) subject to MIPA and March Air Reserve Base review.

- **Six-Foot Tubular Steel Fences** are provided around the Water Quality Basins to provide safety and security for pedestrians walking near the Basins. The Tubular Steel Fence is constructed from steel pickets painted black.
- Screening walls for trucks shall be 12' high (minimum).

Pedestrian and vehicular access gates visible from public areas such as parking lots and public streets shall be constructed of a durable material, such as tubular steel.

Plot Plans must include all site fencing, truck screening wall, and gate details.

### ***Materials***

Walls are to be constructed of materials compatible with the overall design character of the building. Walls shall be poured-in-place concrete or painted tilt-up screen walls. Fences shall be wrought iron or tubular steel. Chain link fencing is not permitted except for security fencing along the runway.

Design elements may include:

- Varied heights
- wall plane offsets
- Scoring or other decorative elements
- Pilasters or distinctive elements.
- Minor changes of material and finishes where appropriate.
- Trellis/vine panels or landscape pockets.



### 4.3.5 Exterior Lighting

Lighting will utilize high efficiency technologies, dark-sky cutoffs, strategic orientation to avoid spillover into adjacent properties, the adjacent runway, and open space areas, and appropriate shielding or recesses to minimize glare and reflections.

Street and parking lot lighting will meet JPA standards.

- Exterior lighting should be unobtrusive and not cause glare or spillover into neighboring properties, especially when within 100 feet of open space or adjacent runways. Lighting fixtures must be fully shielded to direct illumination downward to minimize light pollution impacts.
- Adequate lighting should be provided throughout the site to create an inviting and non-threatening environment. Night lighting of public spaces should be kept to the minimum necessary for safety and security purposes.
- The scale, materials, colors, and design detail of on-site light posts and fixtures should reflect the desired character of the Specific Plan area and the architectural style of the surrounding buildings. Light posts should be appropriately scaled to pedestrians near sidewalks and other areas of pedestrian circulation. Extremely tall light posts and fixtures should be avoided – maximum height is 25 feet. Bollard lighting is encouraged to illuminate walkways without providing spillover.
- Lighting fixtures should be compatible with the architectural style and character of the building. The color, size, placement, and number of fixtures should enhance the overall design and character of the building and site.
- Energy efficient, low voltage lighting is strongly encouraged. Decorative lighting should be low intensity. LED lighting below 2500 Kelvin is also allowed.
- If security lighting is required, fixtures should be hooded, recessed, and/or located in such a manner to only illuminate the intended area.
- Addresses should be visible from streets and illuminated at night.

### 4.3.6 Signage

Signage will be provided in accordance with a Sign Program prepared prior to building permit issuance. The Signage will conform to MIPA Development Code requirements.

# 5 ADMINISTRATION AND IMPLEMENTATION

This chapter discusses the development review procedures by the March JPA and other relevant permitting agencies applicable to the Specific Plan. A process for amendments to the Specific Plan is discussed as well as a process for ~~substantial conformance~~ Administrative Amendment determinations.

The purpose of this chapter is to provide an outline of the steps necessary to implement the Veterans Industrial Park 215 Specific Plan and applicable regulations in coordination with the March JPA and other governing public agencies. This chapter is intended to address each of these elements for the benefit of the development team, the MJPA and other relevant agencies, and interested citizens.

The approval of this Specific Plan, certification of an Environmental Impact Report, and adoption of conditions of approval and a Mitigation Monitoring and Reporting Program (MMRP) will assure that timely mitigation of project impacts takes place at the appropriate milestones and in accordance with project implementation.

### 5.1 ADMINISTRATION

California Government Code (Title 7, Division 1, Chapter 3, Article 8, Sections 65450 et seq.) grants authority to agencies to utilize Specific Plans for purposes of implementing the goals and policies of the General Plan.

This Specific Plan establishes a set of regulations, standards, guidelines, and processes for the proposed development, and shall constitute the zoning for development within the Specific Plan area.

#### 5.1.1 Responsibility

The March JPA's Planning Department, its Director or their designee shall be responsible for administering the Veterans Industrial Park 215 Specific Plan in accordance with the provisions of this Specific Plan document, all governing and applicable state and federal laws, the March JPA General Plan, and the March JPA Development Code.

#### 5.1.2 Applicability

The Veterans Industrial Park 215 Specific Plan serves as the implementation tool for the zoning for the Specific Plan Area. The Specific Plan addresses permitted uses, development standards, and design guidelines.

#### 5.1.3 Enforcement and Interpretation

The JPA shall enforce the provisions of the Specific Plan in the same manner that it enforces the provisions of the General Plan and Development Code.

Whenever the provisions contained in the Specific Plan conflict with the Development Code, the provisions of the Specific Plan shall take precedence. Any ambiguity concerning the content or application of the Veterans Industrial Park 215 Specific Plan shall be resolved by the Planning Director, or their designee. Such interpretations shall take into account the stated goals and intent of the Specific Plan.

#### 5.1.4 Severability

If any portion of this Specific Plan and its regulations are declared to be invalid or ineffective in whole or in part by a court of competent jurisdiction, such decision shall not affect the validity of the remaining portions thereof.

#### 5.1.5 Initial Entitlements

Initial entitlements required for development of the Specific Plan area include the following actions to be taken by the JPA:

- Environmental Impact Report ("EIR") – The Veterans Industrial Park 215 Specific Plan is a discretionary project and is subject to the requirements of the California Environmental Quality

Act ("CEQA"). As part of the approval process for the Specific Plan, an Environmental Impact Report must be considered and certified by the JPA prior to approval of any of the project-related entitlements.

- **General Plan Amendment** – The Project site is presently identified as "Aviation" by the General Plan. An amendment of the General Plan Land Use map will be required to add a "Specific Plan" designation (SP-8) to the underlying Aviation designation. ~~In addition, an amendment to the list of uses within the designation would include the addition of general warehousing/logistics.~~
- **Specific Plan** – The Project site is presently un-zoned. The Veterans Industrial Park 215 Specific Plan is a regulatory document that establishes the zoning, land use designations, development standards, and design guidelines for the entire Specific Plan project area. The Specific Plan will implement the JPA's General Plan. The Specific Plan will be considered by the Joint Powers Commission (JPC) and will be adopted by Ordinance. Tract/parcel maps or plot plans must be in substantial compliance with the adopted Specific Plan.
- **Plot Plan** – ~~The A site development plan for the project, consisting of two buildings of approximately 2,185,618 square feet an industrial/logistics project with proposed structures, parking, landscaping, drainage facilities, and new streets and driveways. If the project changes due to specific tenants after approval, a new or revised plot plan would be processed with MIPA.~~
- **Subdivision Map** – The Subdivision Map is a basic tool for implementation of a Specific Plan. The project's Tentative Parcel Map will create ~~either the individual one or two legal~~ lots for project development, formalize the parcel boundaries, and provide for public rights-of-way for Project access. A Tentative Parcel Map has been prepared (TMP 37220) and will be considered by the JPA concurrently with the review of this Specific Plan. The Parcel Map creates the backbone road rights-of-way, and ~~two either one or two~~ development parcels.
- **Development Agreement/Disposition and Development Agreement** – A statutory development agreement, authorized pursuant to California Government Code Section 65864 et seq., will be processed as part of the approval of this Specific Plan. The development agreement of this Specific Plan will include, among other items, methods for financing acquisition and construction of infrastructure. Such development agreement shall be fully approved before the issuance of the first building permit for this project. In addition, and Disposition and Development Agreement (DDA) will be executed to formalize the land transaction.

### 5.1.6 ~~Substantial Conformance~~ Administrative Amendments

Final development plans for the project may be adjusted or modified based on final design and engineering and the precise development plans of the builder. ~~Substantial Conformance~~ Administrative Amendment is a mechanism to allow the approval of minor modifications for development under the Specific Plan.

Upon the request of Developer for an amendment or modification of any Project Approval, the JPA Planning Director or his/her designee shall determine: (a) whether the requested amendment or modification is minor when considered in light of the Project as a whole; and (b) whether the requested amendment or modification substantially conforms with the material terms of the Specific Plan and the Applicable Law and may be processed administratively. If the JPA Planning Director or his/her designee finds that the requested amendment or modification is both minor and substantially conforms with the material terms of the Specific Plan and the Applicable Law, the amendment or modification shall be determined to be an "Administrative Amendment" and the JPA Planning Director or his/her designee may approve the Administrative Amendment, without public notice or a public hearing.

Written documentation requesting a proposed minor modification/~~substantial conformance~~administrative amendment finding to support an implementing map, site plan, or use permit or modification of conditions of approval must be submitted for the review and approval of the Planning Director or their designee in accordance with Section 9.02.280 and Section 9.02.290 of the March JPA Development Code or as otherwise outlined in the project's Development Agreement.

~~A Substantial Conformance application shall be subject to minor development review procedures. A substantial conformance application may be filed in lieu of an applicable minor development review application, provided that the proposal complies with the limitations described below:~~

- ~~\* — That the proposal is not inconsistent with the expressed intent of the original project approval;~~
- ~~■ That the proposal qualifies as a categorical exemption under the California Environmental Quality Act and/or the proposal is consistent with the environmental determination for the original project and where no further environmental determination is necessary; and~~
- ~~\* — That the proposed modifications do not have the potential to adversely affect surrounding land uses or improvements.~~

### 5.1.7 Amendments

Substantial modifications to the Specific Plan would require an Amendment. A minor modification or adjustment to the Specific Plan listed in the section above would not require a Specific Plan Amendment.

An amendment to the Specific Plan is required if the following occur:

- Changes to the overall Specific Plan boundaries to include ownerships or properties not included in the Specific Plan at the time of approval (changes to planning area boundaries within the Specific Plan boundaries are deemed minor as noted above and would not require an amendment);
- Any increase in the overall development intensity thresholds within the Specific Plan; or
- Any addition of new land uses not contemplated by the Specific Plan's Development Regulations.

### 5.1.8 Appeals

An appeal of any determination, decision, or requirement of the March JPA Planning Director shall be made in conformance with the appeal procedures established by the Development Code Section 9.02.240.

## 5.2 IMPLEMENTATION

### 5.2.1 Adoption

The Veterans Industrial Park 215 Specific Plan will be prepared, submitted, and approved in a manner consistent with California Government Section 65451, as well as Chapter 9.13 of the JPA's Development Code. The Specific Plan will be adopted by Ordinance and shall serve as the zoning for the Veterans Industrial Park 215 project area. The approved Specific Plan project site will be designated on the JPA's General Plan Land Use Diagram and Zoning Map as the Veterans Industrial Park 215 Specific Plan. The land use and development standards identified in this Specific Plan document supersede all zoning regulations to the extent that they would be in conflict with the sections of this Specific Plan.



**5.2.2 Phasing**

Construction of the proposed project, including recordation of final subdivision map(s), and plot plan review may be progressively implemented in stages, provided that vehicular access, public facilities, and infrastructure are constructed to adequately service the development, or as needed for public health and safety.

Any project phasing would:

- Provide for the orderly build-out based upon market demand;
- Provide adequate infrastructure to service the project;
- Phases may occur concurrently so long as the associated infrastructure is provided.

**5.2.3 Maintenance and Ownership**

Maintenance of facilities within the Veterans Industrial Park 215 Specific Plan will be accomplished through a combination of public and private mechanisms. Generally, facilities dedicated to public agencies will be maintained by that agency, while private facilities will be maintained by a private maintenance mechanism. Table 5-1, *Financing, Ownership, and Maintenance* outlines the anticipated program.

A Business Association and/or multiple associations may be formed to address the maintenance of private drives, shared driveways, landscaping, signage, water quality features, and private infrastructure within the Specific Plan.

Table 5-1 Financing, Ownership, and Maintenance			
Improvement	Financing	Ownership	Maintenance
Water System (off-site)	Developer	Public	Public
Water System (on-site)	Developer	Private	Private
Sewer System (off-site)	Developer	Public	Public
Sewer System (on-site)	Developer	Private	Private
Drainage System On-site	Developer	Private	Private
Regional	Developer	Public	Public
Public Street Improvements (Van Buren)	Developer	MJPA	MJPA
Public Street Improvements (Western Way)	Developer/City of Perris with developer mitigation fee	City of Perris	City of Perris
Private Internal Streets and driveways	Developer/Builder	Private	Private
Landscaping within Public Right-of-Way	Developer	MJPA	MJPA/Private

### 5.2.4 Relationship to CEQA

The California Environmental Quality Act (CEQA) classifies a specific plan as a “project” which is subject to environmental review. An Environmental Impact Report (EIR) is required prior to adoption of this Specific Plan to analyze potentially significant environmental impacts of the project, discuss feasible alternatives, and recommend feasible mitigation measures in compliance with the provision of CEQA. This EIR will analyze the Specific Plan and address potential impacts associated with the development of the Specific Plan area. The EIR will include recommended mitigation measures and analyzes implementing actions for the development. The EIR will fulfill the requirements for environmental documentation for most subsequent discretionary and ministerial applications for development within the Specific Plan area.

An approved Mitigation Monitoring Program will insure that the Specific Plan complies with all applicable environmental mitigation and permit requirements. The final approved Mitigation Monitoring program shall be established upon EIR certification by the MIPA.

# 6

## APPENDICES

- A Legal Description
- B General Plan Conformance
- C Land Use Compatibility Plan
- D Landscaping for Airports

**APPENDIX A  
LEGAL DESCRIPTION**

THE LEGAL DESCRIPTION SHOWN HEREON HAS BEEN PREPARED BY THE SURVEYOR OF RECORD FOR THE PURPOSE OF DEPICTING THE AREA TO BE INCLUDED IN THIS SURVEY ONLY. SAID DESCRIPTION IS NOT TO BE USED FOR CONVEYANCE OF TITLE OF ANY NATURE.

THAT PORTION SECTIONS 25, 26, 35 AND 36, TOWNSHIP 3 SOUTH, RANGE 4 WEST, SAN BERNARDINO MERIDIAN, IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, SHOWN AS PARCEL 11 ON MAP FILED IN BOOK 110 PAGES 30 TO 40 INCLUSIVE, OF RECORDS OF SURVEY, IN THE OFFICE OF THE COUNTY RECORDER, OF SAID COUNTY, DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHERLY TERMINUS OF THAT CERTAIN COURSE SHOWN AS HAVING A BEARING AND DISTANCE OF NORTH 30°06'59" WEST 670.29 FEET IN THE EASTERLY BOUNDARY OF SAID PARCEL 11, SAID SOUTHERLY TERMINUS ALSO BEING A POINT ON THE NORTHERLY LINE OF PARCEL MAP NO. 8698, AS PER MAP FILED IN BOOK 37 PAGE 90, OF PARCEL MAPS IN THE OFFICE OF SAID RECORDER; THENCE ALONG SAID NORTHERLY LINE SOUTH 89°53'52" WEST 117.66 FEET; THENCE LEAVING SAID NORTHERLY LINE NORTH 30°09'25" WEST 124.78 FEET TO A LINE PARALLEL WITH 108.00 FEET NORTHERLY, MEASURED AT RIGHT ANGLES, FROM SAID NORTHERLY LINE; THENCE ALONG SAID PARALLEL LINE SOUTH 89°53'52" WEST 1955.75 FEET TO THE EASTERLY BOUNDARY OF CALIFORNIA STATE ROUTE 215, AS SHOWN ON CALIFORNIA DEPARTMENT OF TRANSPORTATION MONUMENTATION MAP 45680 ON FILE IN THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION; THENCE ALONG SAID EASTERLY BOUNDARY NORTH 19°22'43" WEST 4259.32 FEET; THENCE LEAVING SAID EASTERLY BOUNDARY NORTH 77°53'09" EAST 120.23 FEET; THENCE SOUTH 17°20'02" EAST 24.72 FEET; THENCE NORTH 75°40'21" EAST 81.83 FEET; THENCE SOUTH 45°37'50" EAST 766.67 FEET; THENCE NORTH 52°14'06" EAST 614.61 FEET TO THE NORTHWESTERLY TERMINUS OF THAT CERTAIN COURSE SHOWN AS HAVING A BEARING AND DISTANCE OF NORTH 30°07'27" WEST 3507.80 FEET IN THE BOUNDARY OF THE "MARCH AIR RESERVE BASE" AS SHOWN ON MAP FILED IN BOOK 124 PAGES 69 TO 81 INCLUSIVE OF SAID RECORDS OF SURVEY; THENCE ALONG SAID LAST MENTIONED BOUNDARY AS FOLLOWS: SOUTH 30°07'25" EAST 3507.87 FEET; THENCE SOUTH 49°46'59" WEST 73.50 FEET; THENCE SOUTH 39°57'49" EAST 421.43 FEET; THENCE SOUTH 30°06'29" EAST 670.22 FEET TO THE POINT OF BEGINNING.

**APPENDIX B**

**Veteran's Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
Land Use		
<b>Goal 1</b>	<i>Land Use Plan provides for a balanced mix of land uses that contribute to the regional setting, can capitalize on the assets of the Planning Area, while insuring compatibility throughout the Planning Area and with regional plans.</i>	The project provides for logistics/industrial uses. These uses are compatible with the General Plan and the adjacent Perris Valley Commerce Center Specific Plan. The project will include a General Plan Amendment adding a Specific Plan Overlay designation to the Site. Consistent with the Reuse Plan and General Plan, the project will continue to improve the balance of population and employment in the project vicinity, providing an opportunity for residents to work locally, rather than commute to surrounding areas throughout the region.
<b>Policy 1.1</b>	<i>Provide for a mix of land uses which implement the Base Master Reuse Plan for March AFB; offer a variety of employment opportunities; and capitalizes, enhances and expands upon existing physical and economic assets of the Planning Area.</i>	See response to Land Use Goal 1.
<b>Policy 1.2</b>	<i>Develop and maintain a system of land use designations and zoning districts which will provide locations for commercial, business park, manufacturing, aviation, public, and open space uses, and which actuates compatible and synergistic land uses.</i>	See response to Land Use Goal 1.

**APPENDIX B**

**Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
Land Use		
<b>Policy 1.3</b>	<i>Provide for patterns of land use which can be supported by existing and planned circulation, public facilities, and infrastructure system improvements in a manner that will preserve the March JPA’s fiscal capacity.</i>	<p>The project is consistent with the approved General Plan circulation plan. The project will extend Van Buren Boulevard south of the March Air Field Museum. The General Plan designates the Van Buren Boulevard extension as a Major Arterial. This segment of Van Buren Boulevard will be designed as a Modified Secondary Highway, which while providing two traffic lanes, provides 97-feet of right-of-way instead of 100-feet of right-of-way the General Plan assigned to a Major Arterial. An additional 20-foot separation between the edge of sidewalk and screen walls associated with the Veterans Park Specific Plan.</p> <p>In addition, the project EIR will evaluate the utility providers’ ability to serve the project. Any significant impacts to public services or utilities will be mitigated through the environmental review process and prior to development.</p>
<b>Policy 1.4</b>	<i>Use specific and/or master plan processes for the coordinated development of large properties to ensure cohesive, comprehensive development.</i>	<p>The property does not currently have a zoning designation. The General Plan land use is Aviation, which would allow for a variety of uses including hangars, aviation support services, air cargo storage, fixed based operations, and aviation operations services. The Site does not have direct access to flying facilities. As such, industrial facilities described in the Specific Plan will allow for a mix of logistics center uses that could support e-commerce, wholesale, storage, distribution, manufacturing and/or assembly centers. It is anticipated that these uses would support airport-related businesses in the future.</p>
<b>Policy 1.5</b>	<i>Provide for a variety of industrial uses, including heavy manufacturing, light manufacturing, warehousing and distribution, transportation - related, and research and development.</i>	<p>The project will provide a mix of logistics center uses that could support e-commerce, wholesale, storage, distribution, manufacturing and/or assembly centers.</p>
<b>Policy 1.6</b>	<i>Locate and group commercial and industrial uses which are oriented toward regional service/market areas to promote utilization of regional transportation facilities and development-supporting infrastructure.</i>	<p>See response to Land Use Goal 1.</p>

**APPENDIX B**

**Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
Land Use		
<b>Policy 1.9</b>	<i>Plan for compatible land uses within the aircraft noise impact contours depicted in the Air Installation Compatible Use Zones (AICUZ) Report for the airfield use.</i>	The project does not include specific aviation-related facilities, however, the development concept could support airport related businesses. Building height and uses described within the Specific Plan are consistent with the AICUZ guidelines and requirements.
<b>Goal 2</b>	<i>Locate land uses to minimize land use conflict or creating competing land uses, and achieve maximum land use compatibility while improving or maintaining the desired integrity of the Planning Area and subregion.</i>	The project provides the same types of land uses as designated in the General Plan. These land uses are compatible with the surrounding land uses. Incompatible or competing land uses will not be allowed in the project area.
<b>Policy 2.1</b>	<i>Avoid conflicts and incompatibilities between land uses through the use of landscaped setbacks and buffers, site design, site orientation, architectural features, walls or fences, density/intensity reductions, reduced hours of operation for commercial and industrial uses, shielding of lighting, and the like.</i>	The Specific Plan Design Guidelines provide the architectural, signage, walls and fences, lighting, and landscaping standards to help alleviate any land use incompatibilities with the surrounding areas.
<b>Policy 2.3</b>	<i>Support land uses that provide a balanced land use pattern of the Planning Area, and discourage land uses that conflict or compete with the services and/or plans of adjoining jurisdictions.</i>	See response to Land Use Goal 2.
<b>Policy 2.4</b>	<i>Protect the interests of, and existing commitments to adjacent residents, property owners, and local jurisdictions in planning land uses.</i>	See response to Land Use Goal 2.

**APPENDIX B**

**Veteran's Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
Land Use		
<b>Goal 3</b>	<i>Manage growth and development to avoid adverse environmental and fiscal effects.</i>	Development of the project is bound by the terms and conditions of the Development Agreement between the Master Developer and the March JPA. The project will be required to implement the applicable infrastructure and services per the Development Agreement. The Development Agreement accommodates a number of financing strategies to fund public improvements and minimize fiscal impacts.
<b>Policy 3.1</b>	<i>Manage growth so that its rate does not exceed the ability of March JPA or service districts to provide for an acceptable level of public facilities and services.</i>	The project EIR will evaluate the utility providers' ability to serve the project. Any significant impacts to public services or utilities will be mitigated through the environmental review process and prior to development.
<b>Goal 4</b>	<i>Develop an identity and foster quality development within the Planning Area.</i>	The Specific Plan Design Guidelines will provide the architectural, signage, parking, and landscaping standards to achieve the goals of both project identity and quality development.
<b>Policy 4.1</b>	<i>Develop and maintain a land use plan for the Planning Area which proposes compatible land uses to create distinct, identifiable historic, commercial, industrial, public, and aviation areas.</i>	See Response to Land Use Goal 1.
<b>Policy 4.4</b>	<i>Develop a distinctive community identity for commercial, business park and industrial developments that reflect the character and atmosphere of March JPA Planning Area through the use of good planning and design principals, and sound development practices which serve as guidelines for building materials, colors, site design and orientation, and landscaping.</i>	See responses to Land Use Goals 1 and 4.



**APPENDIX B**

**Veteran's Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
Land Use		
<b>Policy 4.7</b>	<i>Develop and enhance the economic climate and create a balanced business community to serve the work force, commerce and industry of the region.</i>	See response to Land Use Goal 1.
<b>Goal 5</b>	<i>Maximize and enhance the tax base and generation of jobs through new, reuse and joint use opportunities.</i>	The project will expand on the large employment center concept started by the adjacent Meridian Specific Plan and Perris Valley Commerce Center Specific Plan. As such, it will provide a substantial enhancement to the tax base.
<b>Policy 5.1</b>	<i>Support the development and establishment of new employment centers and economic development activities that contribute to an improved tax base.</i>	See response to Land Use Goal 5.
<b>Policy 5.2</b>	<i>Encourage and facilitate the creation of public/private partnerships that will invest in, and further the implementation of the March AFB Master Reuse Plan.</i>	Riverside Inland Development, LLC, has entered into a Memorandum of Understanding with March JPA related to development of the property within the Specific Plan area. The applicant, Riverside Inland Development, LLC, has assumed the responsibilities and obligations of the Master Developer for the remaining areas of the former March Air Force Base (which includes this project area) in a Disposition and Development Agreement which will be processed concurrently with the Veterans Industrial Park 215 Specific Plan.
<b>Policy 5.5</b>	<i>Encourage the development of commercial, business park and industrial centers to expand the employment and fiscal base of the March JPA Planning Area and the western Riverside County Subregion.</i>	See response to Land Use Goal 1.

**APPENDIX B**

**Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
Land Use		
<b>Goal 6</b>	<i>Support the continued Military Mission of March Air Reserve Base, and preservation of the airfield from incompatible land use encroachment.</i>	The project is designed to incorporate appropriate uses as defined in the Air Installation Compatible Use Zone (AICUZ) Study, Airport Layout Plan, Department of Defense Instructions and Air Force Instructions.
<b>Policy 6.3</b>	<i>Ensure that plans and development do not interfere, conflict or degrade the military mission of March ARB.</i>	See response to Land Use Goal 6.
<b>Policy 6.4</b>	<i>Ensure that plans and development do not conflict with the long-term needs of the Air Force Reserve in terms of encroachment, noise, accident zone, constraints, etc.</i>	See response to Land Use Goal 6.
<b>Policy 6.5</b>	<i>Ensure that plans and development conform to the draft Comprehensive Land Use Plan for March AFB/March Inland Port.</i>	See response to Land Use Goal 6.
<b>Policy 6.8</b>	<i>Ensure that land uses adhere to both military and civilian Part 77 conical surface criteria, relative to height restrictions.</i>	The project will obtain concurrence letters from the FAA stating, “no hazard to air navigation” for the proposed buildings within the development.
<b>Goal 7</b>	<i>Maximize the development potential as a regional Intermodal Transportation facility to support both passenger and freight related air services</i>	The Veterans Industrial Park 215 Specific Plan includes industrial land uses compatible with the adjacent aviation uses through compliance with runway height limitations, provision of security fencing, and provision of warehousing, and logistics, which could support aviation-related business.
<b>Policy 7.6</b>	<i>Plan for compatible land uses within the aviation area.</i>	See response to Land Use Goal 7.

**APPENDIX B**

**Veteran's Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
Land Use		
<b>Goal 8</b>	<i>Preserve the natural beauty, minimize degradation of the March JPA Planning Area, and provide enhancement of environmental resources and scenic vistas.</i>	There are no known sensitive environmental areas near the site. A jurisdictional drainage channel is present, crossing the project site; impacts to this drainage will be addressed through a permitting process with appropriate agencies. Design features and mitigation measures to minimize the impacts to potential sensitive land uses from the project will be evaluated during the environmental review process.
<b>Policy 8.1</b>	<i>Where practical, revegetate graded area with native plants compatible to the area to prevent erosion.</i>	The project will comply with the requirements of the California Construction General Permit (SWRCB Orders No. 2009-009-DWQ as amended by Order 2010-0014-DWQ and Order 2012-006-DWQ) and employ Best Management Practices (BMPs) to minimize erosion for graded areas.
<b>Goal 10</b>	<i>Avoid undue burdening of infrastructure, public facilities, and services by requiring new development to contribute to the improvement and development of the March JPA Planning Area.</i>	See response to Land Use Goal 3.
<b>Policy 10.1</b>	<i>Require new construction to pay its "fair share" of the cost of providing adequate public services, infrastructure, and facilities for the development.</i>	See response to Land Use Goal 3. The project sponsors will finance the public service extensions to water and sewer lines to serve the project, as well as extending Van Buren Boulevard adjacent to the site. In addition, the project EIR will evaluate traffic and transportation impacts. Any significant impacts to traffic will be mitigated through the environmental review process and prior to development. Further, the project will pay its fair share of the cost of providing services and infrastructure through payment of the development impact fees assessed by the JPA.
<b>Policy 10.2</b>	<i>Require new construction to provide adequate infrastructure to serve the development (i.e., curbs and gutters, sidewalks, street lights, water service, sewer service or septic systems, etc.) prior to initiation of use.</i>	See response to Land Use Goal 3.

**APPENDIX B**

**Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
Land Use		
<b>Policy 10.3</b>	<i>Locate commercial and industrial development in areas where street rights-of-way and capacity are available, as well as sufficient infrastructure and public services.</i>	See response to Land Use Goal 3.
<b>Goal 12</b>	<i>Ensure, plan, and provide adequate infrastructure for all facility reuse and new development, including but not limited to, integrated infrastructure planning, financing and implementation.</i>	See response to Land Use Goal 3.
<b>Policy 12.3</b>	<i>Require new development projects to provide for the extension of infrastructure to serve the development, including over-sizing facilities for future needs.</i>	See responses to Land Use Policy 1.3 and Goal 3.
<b>Goal 13</b>	<i>Secure adequate water supply system capable of meeting normal and emergency demands for existing and future land uses</i>	The project EIR will evaluate the utility providers’ ability to serve the project. Any significant impacts to public services or utilities will be mitigated through the environmental review process and prior to development. A Water Supply Assessment was prepared as part of project entitlement by Western Municipal Water District to evaluate the required water supply for the subject logistics project.
<b>Policy 13.2</b>	<i>Enhance local groundwater supplies through development designs which promote an on-site recharge and minimize impermeable ground coverage with landscaped areas, open space or recreation areas.</i>	See response to Land Use Goal 17. Note that due to the close proximity to the airport runways the project must minimize standing water while addressing water quality requirements for the site. On-site bio-retention basins will treat on-site runoff while minimizing standing water.

**APPENDIX B**

**Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
Land Use		
<b>Goal 14</b>	<i>Establish, extend, maintain and finance a safe and efficient wastewater collection, treatment and disposal system, which maximizes treatment and water recharges, minimizes water use, and prevents groundwater contamination.</i>	The project will provide the necessary conveyance facilities to achieve this goal. The project EIR will evaluate infrastructure requirements, including conveyance and treatment of wastewater.
<b>Policy 14.1</b>	<i>Require all development to adequately collect, treat, and dispose of wastewater in accordance with the Santa Ana Regional Water Quality Control Board requirements.</i>	The project will comply with the March JPA’s NPDES New Development and Redevelopment Guidelines for projects Under the March Joint Powers Authority and the Santa Ana Regional Water Quality Control Board Order No. R8-2002-0011.
<b>Policy 14.2</b>	<i>Require connection to the sewer system for any development occurring on land formerly part of March AFB.</i>	See response to Goal 14 and Land Use Policy 14.1. As shown in Figure 3-6 of the Specific Plan, the project will extend sewer lines to service the project, connecting to the existing sewer lines adjacent to the Specific Plan area.
<b>Policy 14.3</b>	<i>Encourage reuse of reclaimed and treated non-potable water for irrigation and maintenance of recreation areas, landscaping and /open space preservation.</i>	Reclaimed water is not available for use within the project.
<b>Goal 16</b>	<i>Adequate supplies of natural gas and electricity from utility purveyors and the availability of communications services shall be provided within the March JPA Planning Area.</i>	The project EIR will evaluate the utility providers’ ability to serve the project. Any significant impacts to public services or utilities will be mitigated through the environmental review process and prior to development.
<b>Policy 16.1</b>	<i>Where feasible, require new development to underground on-site telecommunication connections.</i>	The project will underground on-site telecommunication connections.

**APPENDIX B**

**Veteran's Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
Land Use		
<b>Goal 17</b>	<i>Adequate flood control facilities shall be provided prior to, and concurrent with, development in order to protect the lives and property within the March JPA Planning Area.</i>	The project will comply with the March JPA's NPDES New Development and Redevelopment Guidelines for projects Under the March Joint Powers Authority and the Santa Ana Regional Water Quality Control Board Order No. R8-2002-0011. A hydrology study and water quality management plan (WQMP) will be prepared for the project. It is not presumed that project implementation would have a substantial impact on water quality standards or waste discharge requirements. The project will provide for a drainage plan to convey on-site flows to the existing drainage areas downstream of the project. Any significant impacts to hydrology and water quality will be mitigated through the environmental review process and prior to development.
<b>Policy 17.1</b>	<i>Provide for the adequate drainage of storm runoff to protect the lives and property within the Planning Area.</i>	See response to Land Use Goal 17.
<b>Policy 17.2</b>	<i>Monitor and maintain drainage and flood control facilities to ensure adequate capacity to support the land use plan.</i>	See response to Land Use Goal 17.
<b>Policy 17.3</b>	<i>Require new development to construct new or upgrade existing drainage facilities to accommodate the additional storm runoff caused by the development.</i>	The project's hydrology study evaluates the system required to capture and convey on-site runoff. Site runoff will be captured and detained in three on-site basins, located in each of the two planning areas within the Specific Plan area. Water will be detained, treated, and released at a rate consistent with the existing condition.

**APPENDIX B**

**Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
<b>Transportation</b>		
<b>Goal 1</b>	<i>Establish and provide for a comprehensive transportation system that captures the assets and opportunities of the planning area, existing transportation facilities, and planned transportation facilities for the future growth and development of the planning area and sub-region.</i>	The project is located to the east of the Meridian Specific Plan area. The existing infrastructure for the Meridian development will be extended to the east to support the project, including extension of Van Buren Boulevard from its existing terminus to the extension of Western Way on the Specific Plan’s southern edge. This road extension is consistent with the planned roadway network in the General Plan Circulation Element.
<b>Policy 1.1</b>	<i>Plan for a mix of transportation modes aimed at effective use of resources, both physical infrastructure and natural energy resources.</i>	See responses to Transportation Goals 1 and 2.
<b>Policy 1.2</b>	<i>Design transportation improvements which are compatible with the natural environment. Xeriscape and drought tolerant landscaping techniques should be used for all parkway and median plantings. Where feasible non-potable water should be used for irrigation purposes.</i>	See responses to Land Use Policy 14.3 and Resource Management Policy 1.5.  The Specific Plan’s landscape plan illustrates the proposed landscaping within the Specific Plan Area and in the proposed roadways supporting the project. The plant palette outlined in Table 4-1 of the Specific Plan is comprised of drought tolerant landscape materials.
<b>Policy 1.4</b>	<i>Roadway system shall inter-relate with the components of the multi-faceted transportation system that will assist with the synergistic value of each element’s effectiveness (i.e., bike lockers at the Metrolink station, with bike lanes emanating there from).</i>	See responses to Transportation Goals 1 and 2.  No bikeways are identified in the General Plan as part of the extension of Van Buren Boulevard east of I-215. The proposed Van Buren extension includes an on-street bicycle lane as part of the project improvements, and on-site bicycle parking is provided.

**APPENDIX B**

**Veteran's Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
<b>Transportation</b>		
<b>Goal 2</b>	<i>Build and maintain a transportation system which capitalizes on the multi-faceted elements of transportation planning and systems, designed to meet the needs of the planning area, while minimizing negative effects on air quality, the environment and adjacent land uses and jurisdictions.</i>	No bikeways are identified in the General Plan as part of the extension of Van Buren Boulevard east of I-215. The proposed Van Buren extension includes an on-street bicycle lane as part of the project improvements, and on-site bicycle parking is provided.
<b>Policy 2.6</b>	<i>FAA Standards, military AICUZ, and appropriate Comprehensive Land Use Plan for March Airfield shall be upheld and supported to encourage and realize a safe environment in and around the aviation field.</i>	FAA standards and AICUZ requirements, as well as Department of Defense Instructions and Air Force Instructions, will be complied with. The project's land use plan and development regulations include height limitations associated with the 35-foot building restriction line and the 7:1 height requirements. In addition, the proposed uses comply with the Zone B2 occupancy and use restrictions. Security fencing will be provided adjacent to the runway. In addition, no standing water exceeding the airport's 48 hour criteria will be allowed as part of the project's drainage plans.
<b>Policy 2.7</b>	<i>On-street parking shall be de-emphasized throughout the planning area to permit maximum capacity of roadways to be actuated by vehicular and bicycle transportation modes.</i>	The project will provide adequate off-street parking to limit the potential for on-street parking. This will allow for more capacity on the roadways for sidewalks and landscaping.
<b>Policy 2.8</b>	<i>Street improvements shall be designed in a comprehensive manner to include parkway facilities, pedestrian walkways, commuter bike lanes, signing, lighting noise and air quality factors, as applicable.</i>	The project will comply with the roadway standards within the Circulation Plan associated with this Specific Plan and its associated Design Guidelines. The Specific Plan specifies the cross-sections for public roadways which include sidewalks, medians, and landscaping. The Design Guidelines provide the architectural, signage, parking, and landscaping standards to achieve the goals of both project identity and quality development. For the Van Buren Boulevard extension to the east of I-215, the General Plan does not include on-street bike lanes.



**APPENDIX B**

**Veteran's Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
<b>Transportation</b>		
<b>Goal 3</b>	<i>Develop a transportation system that is safe, convenient, efficient and provides adequate capacity to meet local and regional demands.</i>	This project will construct an internal driveway/private drive network and provide for the extension of Van Buren Boulevard from its existing terminus to a planned extension of Western Way on the Specific Plan's southern edge, based on future demand and the General Plan circulation element. Transportation improvements will be constructed and phased as determined by the project EIR's traffic analysis. The project EIR will evaluate traffic and transportation impacts. Any significant impacts to traffic will be mitigated through the environmental review process and prior to development.
<b>Policy 3.1</b>	<i>Follow standards for transportation element roadways in designing and constructing street improvements.</i>	See response to Transportation Policy 2.8.
<b>Goal 4</b>	<i>Provide a balanced transportation system that ensures the safe and efficient movement of people and goods throughout the planning area, while minimizing the use of land for transportation facilities.</i>	project internal drives and adjacent public roadways (Van Buren Boulevard and Western Way extensions) will be sized to accommodate projected future traffic in an efficient manner.
<b>Policy 4.2</b>	<i>All streets shall be constructed in accordance with planning area's standard street classifications. Modifications within the Northeast Planning Subarea to the standard street classifications may be allowed on a limited basis to preserve the integrity of the area and facilities.</i>	See response to Transportation Policy 2.8.

**APPENDIX B**

**Veteran's Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
<b>Transportation</b>		
<b>Policy 4.10</b>	<i>Work with the City of Perris to plan for an arterial roadway on the east frontage of I-215 between Van Buren Boulevard and Oleander Avenue, in order to preserve future options for developing a passenger or air cargo terminal on the west side of the runway.</i>	The proposed extension of Van Buren Boulevard on the eastern edge of I-215, connecting with a proposed extension of Western Way in the City of Perris, implements this requirement.
<b>Goal 6</b>	<i>Establish vehicular access control policies in order to maintain and insure the effectiveness and capacity of arterial roadways.</i>	project internal roadways will be designed in accordance with the "County Road Improvement Standards and Specifications," published by the County of Riverside, and take into account additional landscaping requirements established in the Riverside County Integrated Plan County standards, and implement appropriate intersection and driveways intervals on arterial roadways.
<b>Policy 6.2</b>	<i>Access to an arterial road shall be limited to one point for every 300 feet of frontage or one point for parcels with less than 300 feet of frontage.</i>	The Specific Plan area includes approximately 5,500 linear feet of frontage on the proposed extension of Van Buren Boulevard, allowing for approximately 18 access points. The Specific Plan land use includes six points of access and is thus consistent with this requirement.
<b>Goal 7</b>	<i>Facilitate and develop transportation demand management and transportation systems management programs, and use of alternate transportation modes.</i>	Transportation Demand Management (TDM) strategies will be implemented to shift trips outside the standard commuting hours and/or to non-"drive alone" modes of travel. This is accomplished through various employer-initiated measures, such as flexible working hours, encouragement of carpooling, and facilitating access for non-motorized (i.e., bicycling or walking) modes of travel.
<b>Policy 7.5</b>	<i>Provide a system of bicycle facilities (paths, lanes and routes) in conjunction with circulation system roadway improvements.</i>	No bikeways are identified in the General Plan as part of the extension of Van Buren Boulevard east of I-215. The proposed Van Buren extension includes an on-street bicycle lane as part of the project improvements, and on-site bicycle parking is provided.

**APPENDIX B**

**Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
<b>Transportation</b>		
<b>Goal 8</b>	<i>Adequate, affordable, equitably distributed and energy efficient public and mass transit services which promote the mobility to, from, and within the planning area shall be provided.</i>	See response to Transportation Goal 2. The local transit system of bus stops and bus shelters will be approved by the Riverside Transit Agency (RTA) as appropriate along the proposed extension of Van Buren Boulevard and Western Way.
<b>Policy 8.1</b>	<i>Evaluate transportation alternatives with project design, development and implementation.</i>	See responses to Transportation Goals 1 and 2.
<b>Goal 9</b>	<i>Develop measures which will reduce the number of vehicle-miles traveled during peak travel periods.</i>	See response to Land Use Goal 1. The project will provide a large employment base, which will provide an opportunity for residents in the vicinity to work locally, rather than commute to Los Angeles or Orange Counties. This improved Jobs/housing balance will help reduce vehicle miles traveled.
<b>Policy 9.1</b>	<i>Provide incentives to employers who encourage carpooling and vanpooling for employees.</i>	See response to Transportation Goal 7.
<b>Policy 9.2</b>	<i>Provide preferential parking for carpools and vanpools, where appropriate.</i>	The Specific Plan development regulations require a minimum of 5% carpool/vanpool parking spaces within the Specific Plan area.
<b>Goal 10</b>	<i>Regulate the travel of trucks on March JPA Planning Area streets.</i>	The project is designed to accommodate truck traffic. Western Way is a designated truck route in the City of Perris, and Van Buren Boulevard is a truck route as designated in the General Plan.

**APPENDIX B**

**Veteran's Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
<b>Transportation</b>		
<b>Policy 10.1</b>	<i>Establish a truck route system which designates truck and commercial vehicle routes and provides adequately sized and designed roadways to meet the needs of trucks and commercial vehicles. This will eliminate truck and commercial vehicle traffic through inappropriate areas of the March JPA Planning Area.</i>	See response to Transportation Goal 10.
<b>Policy 10.2</b>	<i>Clearly sign designated truck routes and identify maximum weight limitations on these routes.</i>	See response to Transportation Goal 10. This requirement would be implemented as part of construction plans for the public roadways developed as part of the project (Van Buren Boulevard and Western Way).
<b>Goal 11</b>	<i>Adequate off-street parking for all land uses shall be provided which requires adequate on-site parking to prevent spill over on the adjacent street system.</i>	The project will provide adequate parking to limit the potential for parking spillover on to streets. Parking will be provided in accordance with the Development Regulations established by the Specific Plan. Parking ratios have been provided based on square footage. Parking ratios for car/vanpools, bicycle parking, and charging stations have also been provided in the Specific Plan development regulations.
<b>Policy 11.1</b>	<i>Provide for adequate parking facilities for all uses.</i>	See response to Transportation Goal 11.
<b>Policy 11.4</b>	<i>Require all new development to provide adequate off-street parking based on expected parking needs</i>	See response to Transportation Goal 11.
<b>Policy 11.5</b>	<i>Provide adequate loading areas within off-street parking areas for all commercial and manufacturing land uses.</i>	The project will provide adequate loading areas associated with the proposed logistics/industrial buildings.

**APPENDIX B**

**Veteran's Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
<b>Transportation</b>		
<b>Goal 12</b>	<i>Plan for and seek to establish an area-wide system of bicycling trails, with linkages within the planning area and with adjacent jurisdictions, and in compliance with sub-regional plans.</i>	No bikeways are identified in the General Plan as part of the extension of Van Buren Boulevard east of I-215. The proposed Van Buren extension includes an on-street bicycle lane as part of the project improvements, and on-site bicycle parking is provided.
<b>Policy 12.7</b>	<i>Require sidewalks on both sides of all streets. The March JPA encourages alternate designs including parkways and meandering and enhanced paving.</i>	As shown in the street cross sections of the Specific Plan's Circulation Plan, sidewalks are provided on the project-frontage of Van Buren Boulevard, and right of way accommodating sidewalks is provided on both Van Buren Boulevard and Western Way extensions.
<b>Goal 13</b>	<i>Promote, preserve, and protect the joint use of the aviation field by the Air Force Reserves and civilian aviation.</i>	The project will not impact the use of the air field in any way.
<b>Policy 13.6</b>	<i>Protect flight paths from inappropriate development encroachment.</i>	The project is designed to incorporate appropriate uses as defined in the Air Installation Compatible Use Zone (AICUZ) Study, Airport Layout Plan, Department of Defense Instructions and Air Force Instructions
<b>Policy 13.8</b>	<i>Adhere to approved airport layout plans approved by the Joint Powers Commissions and recognized by the FAA.</i>	See response to Policy 13.6 above.
<b>Goal 15</b>	<i>In accordance with state and federal law, promote and provide mobility for the disabled.</i>	Development plans shall take into account the accessibility requirements of the Americans with Disability Act (ADA).
<b>Policy 15.1</b>	<i>Require that all development comply with the requirements of the state and federal law for the disabled. Requirements may include ramps at street corners, access to public buildings, traffic signal timing and the like.</i>	See response to Transportation Goal 15.

**APPENDIX B**

**Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
<i>Noise</i>		
<b>Goal 1</b>	<i>Ensure that land uses are protected from excessive and unwanted noise.</i>	project development shall be consistent with the land use and noise limitations established in the AICUZ study and the Riverside County Airport Land Use Plan and MJPA Development Code. Industrial uses are consistent with the noise limitations of the AICUZ study.
<b>Policy 1.1</b>	<i>Establish acceptable limits of noise for various land uses throughout the March JPA Planning Area. Future development that could increase ambient noise levels shall be required to mitigate the anticipated noise increase, to the extent possible.</i>	The project EIR will evaluate noise impacts. The Specific Plan area is located adjacent to the airport runway, an inherently noisy environment. Any significant impacts from project noise will be mitigated through the environmental review process and prior to development. Industrial uses are consistent with the noise limitations of the AICUZ study.
<b>Policy 1.3</b>	<i>Encourage good acoustical design in new construction.</i>	See response to Noise Policy 1.1.
<b>Goal 2</b>	<i>Minimize incompatible noise level exposures throughout the Planning Area, and where possible, mitigate the effect of noise incompatibilities to provide a safe and healthy environment.</i>	No sensitive uses are located in proximity to the proposed Specific Plan area. Design features and mitigation measures to minimize noise impacts from the project will be evaluated during the environmental review process. Incompatible land uses will not be allowed in the project area.
<b>Policy 2.4</b>	<i>March JPA shall evaluate noise sensitivity and noise generation when considering land use projects and transportation improvement projects, and where appropriate mitigation measures shall be employed.</i>	See response to Noise Goal 2.

**APPENDIX B**

**Veteran's Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
Noise		
<b>Goal 3</b>	<i>Work toward the reduction of noise impacts from vehicular traffic, and aviation and rail operations.</i>	See response to Noise Goal 2.
<b>Policy 3.4</b>	<i>Where appropriate, noise mitigation measures shall be incorporated in the design and approval of development property located adjacent to aviation and rail facilities.</i>	Noise mitigation, if required, will be incorporated into the project based on the noise studies prepared as part of the project's Environmental Impact Report.
<b>Policy 3.7</b>	<i>Limit trucking operations to appropriate routes, times and speeds.</i>	The project is designed to accommodate truck traffic. Western Way is a designated truck route in the City of Perris, and Van Buren Boulevard is a truck route as designated in the General Plan. Standard speeds would apply on public truck routes.
<b>Policy 3.8</b>	<i>Appropriate muffling systems for construction equipment and operations shall be required, as necessary.</i>	The project EIR will evaluate noise impacts associated with construction. Any significant impacts from project construction noise will be mitigated through the environmental review process and prior to development.

General Plan Goal/Policy		Consistency
Air Quality		
<b>Goal 1</b>	<i>Promote alternative modes of travel.</i>	See response to Transportation Goal 2.
<b>Policy 1.3</b>	<i>Support trip-reduction programs, such as longer work days, shorter week work schedules.</i>	See response to Transportation Goal 7.

**APPENDIX B**

**Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
Air Quality		
<b>Goal 2</b>	<i>Reduce emissions associated with vehicle miles traveled by enhancing the jobs/housing balance of the subregion of western Riverside County.</i>	The project will provide an employment base, which will provide an opportunity for residents in the vicinity to work locally, rather than commute to Los Angeles or Orange Counties. This improved Jobs/housing balance will help reduce vehicle miles traveled.
<b>Policy 2.1</b>	<i>Create an employment center within the housing rich environment of the subregion.</i>	See responses to Air Quality Goal 1 and Transportation Goal 9.
<b>Goal 3</b>	<i>Reduce air pollution through proper land use, transportation and energy use planning.</i>	See responses to Transportation Goals 2 and 7.
<b>Policy 3.1</b>	<i>Locate ancillary uses within business and employment centers to reduce the number of vehicle trips and lessen the vehicle miles traveled.</i>	The Specific Plan development regulations allow for ancillary office and retail uses in association with the proposed logistics uses. Inclusion of these ancillary uses will reduce the need for outside vehicular trips.
<b>Policy 3.2</b>	<i>Locate service uses and facilities in convenient proximity to employment and business center areas to encourage pedestrian or alternative transit to reduce the number of vehicle trips.</i>	See responses to Land Use Goal 1 and Transportation Goal 9.
<b>Policy 3.3</b>	<i>Develop a bike lane network that will link the bike lanes to residential areas adjacent to the Planning Area and Metrolink Stations to encourage non-motorized travel within the planning area.</i>	No bikeways are identified in the General Plan as part of the extension of Van Buren Boulevard east of I-215. The proposed Van Buren extension includes an on-street bicycle lane as part of the project improvements, and on-site bicycle parking is provided.
<b>Policy 3.4</b>	<i>Encourage ride share programs.</i>	See response to Transportation Goal 7.



**APPENDIX B**

**Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
<i>Air Quality</i>		
<b>Policy 3.5</b>	<i>Parking facilities shall be designed to safely accommodate and support alternative modes of transportation and preferential location of alternative fuel vehicles and mass transit services.</i>	See response to Transportation Goal 2. In addition, the project will include charging stations for electric vehicles within the parking areas of the plan consistent with CAL Green standards.
<b>Goal 5</b>	<i>Maximize the effectiveness of air quality control programs through coordination with other governmental entities.</i>	The project will comply with South Coast Air Quality Management District rules and regulations.
<b>Policy 5.5</b>	<i>Review development projects to determine the potential air quality impacts and provide appropriate mitigation, where necessary.</i>	The project EIR will evaluate air quality impacts. Any significant impacts to air quality from the project will be mitigated through the environmental review process and prior to development.
<b>Goal 6</b>	<i>Reduce emissions associated with vehicle/engine use.</i>	See responses to Land Use Goal 1 and Transportation Goal 9.
<b>Policy 6.3</b>	<i>Encourage diversion of peak hour truck traffic, whenever feasible, to off-peak periods to reduce roadway congestion and associated emissions.</i>	See response to Transportation Goal 10.
<b>Policy 6.5</b>	<i>Encourage trucks operating within March JPA Planning Area to maintain safety equipment and operate at safe speeds so as to reduce the potential for accidents which create congestion and related emissions.</i>	The project EIR will evaluate air quality impacts. Any significant impacts to air quality from the project will be mitigated through the environmental review process and prior to development.

**APPENDIX B**

**Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
<i>Air Quality</i>		
<b>Goal 7</b>	<i>Reduce emissions associated with energy consumption.</i>	Buildings shall be designed to reduce energy usage through various measures (such as energy efficient lighting and glazing, using lighter colored roofing materials, orienting buildings north and increasing wall insulation above Title 24 requirements, etc.) The project’s EIR will analyze the impacts associated with energy consumption and proposed mitigation measures to reduce impacts.
<b>Policy 7.1</b>	<i>Support the use of energy-efficient equipment and design in the March JPA Planning Area for facilities and infrastructure.</i>	See response to Air Quality Goal 7.
<b>Policy 7.3</b>	<i>Support passive solar design in new construction.</i>	See response to Air Quality Goal 7.
<b>Policy 7.4</b>	<i>Support recycling programs which reduce emissions associated with manufacturing and waste disposal.</i>	Recycling of materials will be a component of the waste management program of uses within the Specific Plan area.
<b>Policy 7.5</b>	<i>Support drought-resistant vegetation in landscaping areas to reduce energy needed to pump water.</i>	The specific Plan includes a landscape plan and plant palette for development within the Specific Plan area. The project’s plant materials include predominantly drought tolerant vegetation.
<b>Goal 8</b>	<i>Reduce air pollution emissions and impacts through siting and building design.</i>	See response to Air Quality Goal 7. Design features and mitigation measures to minimize the impacts to air quality from the project will be evaluated during the environmental review process.
<b>Policy 8.1</b>	<i>Support the use of low polluting construction materials and coatings.</i>	The project EIR will evaluate air quality impacts from construction. Any significant impacts to air quality from the project will be mitigated through the environmental review process and prior to development.

**APPENDIX B**

**Veteran's Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
<b>Air Quality</b>		
<b>Goal 9</b>	<i>Reduce fugitive dust and particulate matter emissions.</i>	Trucks hauling dirt, sand, gravel or soil are to be covered or should maintain at least two feet of freeboard in accordance with Section 23114 of the California Vehicle Code. Where feasible, construction access roads to the main roads should be paved to avoid dirt being carried on to the roadway or track-out devices should be installed. In addition, the project EIR will evaluate impacts associated with fugitive dust and particulate matter during construction and proposed mitigation measures to reduce or eliminate impacts.
<b>Policy 9.1</b>	<i>Require all feasible fugitive dust reduction techniques to be utilized during construction activities.</i>	See response to Air Quality Goal 9.

General Plan Goal/Policy		Consistency
<b>Housing</b>		
<p>The General Plan does not allow for housing opportunities within the March JPA Planning Area due to incompatible uses with the airfield, the need to focus on the reestablishment of the numerous jobs lost due to base realignment, and the housing rich environment of Western Riverside County. The project maintains consistency with the General Plan's absence of a residential land use designation within the Specific Plan area.</p>		

**APPENDIX B**

**Veteran's Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
<b>Resource Management</b>		
<b>Goal 1</b>	<i>Conserve and protect surface water, groundwater, and imported water resources.</i>	The project will be constructed to mitigate impacts to the existing drainage channel. Furthermore, the project EIR will evaluate impacts to hydrology and water supply from the project. Any significant impacts to hydrology or water supply from the project will be mitigated through the environmental review process and prior to development.
<b>Policy 1.1</b>	<i>Where possible, retain local drainage courses, channels and creeks in their natural condition.</i>	See response to Resource Management Goal 1. The on-site drainage is related to regional water flows across the site and is not a natural stream course. The proposed grading plan impacts this drainage to facilitate development of the site and to provide an alternative drainage conveyance plan for regional flows. Impacts will be mitigated in accordance with agency permitting requirements.
<b>Policy 1.2</b>	<i>Protect groundwater and surface water resources from depletion and sources of pollution.</i>	See responses to Land Use Goal 17 and Resource Management Goal 1.
<b>Policy 1.4</b>	<i>Require development to conserve water resources, including the use of water-efficient plumbing fixtures and irrigation systems.</i>	See response to Air Quality Goal 7.
<b>Policy 1.5</b>	<i>Conserve imported water by requiring water conservation techniques, water-conserving and recycling processes, drought-resistant landscaping, and reclaimed water for irrigation, when available and appropriate.</i>	The project will comply with the Specific Plan's Design Guidelines. The Design Guidelines require the use of drought-resistant landscaping by the project. Reclaimed water will be used if available.
<b>Policy 1.6</b>	<i>Promote the use of drought tolerant landscaping in development, and encourage the use of reclaimed water for irrigation in parks, golf courses, and industrial uses, as well as for other urban uses, whenever feasible and where legally permitted.</i>	See response to Resource Management Policy 1.5. The Specific Plan includes a landscape plan and plant palette for development within the Specific Plan area. The project's plant materials include predominantly drought tolerant vegetation. The project does not contain parks.

**APPENDIX B**

**Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
<b>Resource Management</b>		
<b>Policy 1.8</b>	<i>Assure that development projects comply with regulatory agency requirements, including federal, state and regional regulations.</i>	The project shall be required to obtain a Clean Water Act (CWA) Section 404 permit (regulated by the U.S Army Corps of Engineers), a Streambed Alteration Agreement (regulated by the California Department of Fish and Wildlife), a CWA Section 401 Water Quality Certification (regulated by the Regional Water Quality Control Board) to impact waters of the U.S/State.
<b>Goal 2</b>	<i>Control flooding to reduce major losses of life and property.</i>	See response to Land Use Goal 17.
<b>Policy 2.3</b>	<i>Ensure that development does not divert storm water run off onto adjacent properties, or cause alterations of natural drainage courses that cannot be adequately handled by flood control improvements installed coincident with the development.</i>	See response to Land Use Goal 17.
<b>Policy 2.5</b>	<i>To the greatest extent possible, require development to use master flood control facilities and limit use of interim drainage facilities or open channels.</i>	The drainage plan for the project will utilize the existing off-site open channel as a downstream conveyance system as well as a box culvert system on the project’s western edge along Van Buren Boulevard. See response to Land Use Goal 17.
<b>Goal 3</b>	<i>Conserve and protect significant land forms, important watershed areas, mineral resources and soil conditions.</i>	The project will not impact any significant landforms or mineral resources. The project EIR will evaluate impacts to hydrology, geology and soils from the project. Any significant impacts to hydrology, geology and soils from the project will be mitigated through the environmental review process and prior to development.

**APPENDIX B**

**Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
<b>Resource Management</b>		
<b>Policy 3.1</b>	<i>Conserve hillsides and rock outcroppings in the planning area through the use of master-planned developments which create a “Campus-like” setting, and encourage the creative siting of building areas as a means of retaining natural areas and open space.</i>	The project will be constructed in a flat area with no rock outcroppings and will not impact any of the area’s hillsides.
<b>Policy 3.5</b>	<i>Require and practice proper soil management techniques to reduce erosion, sedimentation and other soil-related problems.</i>	See response to Land Use Policy 8.1. A SWPPP and Erosion Control Plan will be required for the project to reduce erosion and other soil related problems. In addition, all grading will be reviewed by a soils consultant as part of grading permit preparation.
<b>Policy 3.6</b>	<i>Control erosion during and following construction through proper grading techniques, vegetation replanting, and the installation of proper drainage control improvements.</i>	See response to Land Use Policy 8.1 and Resource Management Policy 3.5
<b>Policy 3.7</b>	<i>Require erosion control measures such as binders, revegetation, slope covers, and other practices which reduce soil erosion due to wind and water.</i>	See response to Land Use Policy 8.1 and Resource Management Policy 3.5
<b>Goal 4</b>	<i>Conserve energy resources through use of available energy technology and conservation practices.</i>	See response to Air Quality Goal 7.
<b>Policy 4.1</b>	<i>Implement energy performance requirements established under the California Administration Code Title 24 Energy Conservation and Insulation Regulations.</i>	See response to Air Quality Goal 7.

**APPENDIX B**

**Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
<b>Resource Management</b>		
<b>Policy 4.2</b>	<i>Encourage innovative building, site design and orientation techniques which minimize energy use by taking advantage of sun/shade patterns, prevailing winds, landscaping, and building materials available to control energy usage.</i>	See response to Air Quality Goal 7.
<b>Policy 4.3</b>	<i>Encourage the use and development of alternative and innovative energy resources and energy conservation techniques, where practical.</i>	See response to Air Quality Goal 7.
<b>Goal 5</b>	<i>Conserve and protect significant stands of mature trees, native vegetation, and habitat within the planning area.</i>	The project EIR will evaluate Impacts to biological resources from the project. Any significant impacts to biological resources from the project will be mitigated through the environmental review process and prior to development. The project would comply with the federal, state and local regulations regarding impacts to sensitive biological resources.
<b>Policy 5.1</b>	<i>Where practical, conserve important plant communities and habitats such as riparian areas, wetlands, significant tree stands, and species by using buffers, creative site planning, revegetation and open space easements/dedications.</i>	See responses to Resource Management Goals 1 and 5.
<b>Policy 5.2</b>	<i>Encourage the planting of native species of trees and other drought-tolerant vegetation.</i>	See response to Resource Management Policy 1.5.

**APPENDIX B**  
**Veteran's Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
<b>Resource Management</b>		
<b>Policy 5.4</b>	<i>In areas that may contain important plant and animal communities, require development to prepare biological assessments identifying species types and locations and develop measures to preserve recognized sensitive species, as appropriate.</i>	See responses to Resource Management Policy 1.8 and Goal 5. The site is not located in an area with important plant and animal communities.
<b>Policy 5.6</b>	<i>Work with state, federal and local agencies in the preservation and/or mitigation of recognized sensitive vegetation and wildlife in March IPA Planning Area.</i>	See response to Resource Management Policy 1.8.
<b>Goal 6</b>	<i>Provide an effective and efficient waste management system for solid and hazardous wastes that is financially and environmentally responsible.</i>	The project shall comply with appropriate and applicable regulations and standards with respect to the management of solid and hazardous wastes.
<b>Policy 6.4</b>	<i>Coordinate with regulatory agencies in assuring that future development handles and disposes of hazardous materials in compliance with applicable regulations.</i>	See response to Safety Risk/Management Goal 4.
<b>Goal 7</b>	<i>Promote cultural awareness through preservation of the planning area's historic, archaeological and paleontological resources.</i>	The project is not anticipated to impact significant historic, archaeological or paleontological resources. The project EIR will comply with the requirements of AB52 and SB18 related to Tribal consultation.



**APPENDIX B**

**Veteran's Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
<b>Resource Management</b>		
<b>Policy 7.5</b>	<i>Require development proposals that are located on or near archaeological or paleontological resources to provide a cultural resources study that assesses potential impacts to the resource as a result of the proposed development. The report will include measures to avoid destruction of any significant cultural resources.</i>	See response to Resource Management Goal 7, above
<b>Policy 7.6</b>	<i>Require the preservation of identified cultural resources to the extent possible, prior to development, through dedication, removal, transfer, reuse, or other means.</i>	See response to Resource Management Goal 7, above
<b>Goal 9</b>	<i>Create a network of open space areas and linkages throughout the Planning Area that serves to preserve natural resources, protect health and safety, contributes to the character of the community, provide active and passive recreational use, as well as visual and physical relief from urban development.</i>	This goal is not applicable to the project, as the site is located between the airport runway and the freeway (I-215) and lacks the potential for significant open space linkages.
<b>Policy 9.8</b>	<i>Enforce the standards of the military and FAA relative to aviation hazard areas to protect the use of the aviation field, and use of property within its vicinity.</i>	The project is designed to incorporate appropriate uses as defined in the Air Installation Compatible Use Zone (AICUZ) Study, Airport Layout Plan, Department of Defense Instructions and Air Force Instructions. The project's land use plan and development regulations include height limitations associated with the 35-foot building restriction line and the 7:1 height requirements. In addition, the proposed uses comply with the Zone B occupancy and use restrictions. Security fencing will be provided adjacent to the runway. In addition, no standing water exceeding the airport's criteria will be allowed as part of the project's drainage plans.

**APPENDIX B**

**Veteran's Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
<b>Resource Management</b>		
<b>Goal 10</b>	<i>Establish standards for scenic corridors, trails and vistas that contribute to the quality of the planning area.</i>	Not applicable. No established scenic corridors or planned trails are present adjacent to the Specific Plan area.
<b>Policy 10.4</b>	<i>Use design standards for transportation facilities that include street trees, pedestrian walkways, bicycle lanes, signing, lighting and setbacks to complement and enhance the character of the planning area.</i>	The Specific Plan includes landscape sections and a landscape plan that includes street trees and pedestrian walkways as well as landscaped setbacks.

General Plan Goal/Policy		Consistency
<b>Safety/Risk Management</b>		
<b>Goal 1</b>	<i>Minimize injury and loss of life, property damage, and other impacts caused by seismic shaking, fault rupture, ground failure, and landslides.</i>	No Fault Rupture Hazard Zone or Alquist-Priolo Earthquake Fault Zone, as designated by the Department of Conservation (DOC), exist within the project site based on the projects soils report and the General Plan. Construction of the project would be required to meet California Building Code (CBC) standards. Additionally, the March JPA would review and approve the plans and specifications of the project to ensure compliance with the provisions of the CBC and Title 24, which regulates building standards, Title 24 is administered by the California Building Standards Commission, which, by law, is responsible for coordinating all building standards.
<b>Policy 1.1</b>	<i>Require geological and geotechnical investigations in areas of potential seismic or geologic hazards as part of the environmental and development review process. Require mitigation of seismic or geologic hazards to the satisfaction of the responsible agencies.</i>	Construction of the project would be required to meet California Building Code (CBC) standards. Additionally, the March JPA would review and approve the plans and specifications of the project to ensure compliance with the provisions of the CBC and Title 24, which regulates building standards, Title 24 is administered by the California Building Standards Commission, which, by law, is responsible for coordinating all building standards.

**APPENDIX B**

**Veteran's Industrial Park 2.15 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
<b>Safety/Risk Management</b>		
<b>Policy 1.2</b>	<i>Ensure all grading plans comply with the Uniform Building Code (UBC) and California Building Code including, if necessary, requiring preliminary investigations of development sites by a State-registered geotechnical engineers and certified engineering geologists.</i>	Construction of the project would be required to meet California Building Code (CBC) standards. Additionally, the March JPA would review and approve the plans and specifications of the project to ensure compliance with the provisions of the CBC and Title 24, which regulates building standards, Title 24 is administered by the California Building Standards Commission, which, by law, is responsible for coordinating all building standards.
<b>Goal 2</b>	<i>Minimize grading and otherwise changing the natural topography, while protecting the public safety and property from geologic hazards.</i>	The project will be constructed in a relatively flat area and will not impact the hillsides or rock outcroppings. The project will incorporate grading development standards and recommendations, which will minimize any potential geotechnical and site development constraints that occur on-site.
<b>Policy 2.1</b>	<i>Discourage any grading beyond that which is necessary to create adequate building pads area.</i>	See response to Safety Risk/Management Goal 2.
<b>Goal 3</b>	<i>Minimize injury, loss of life, property damage, and economic and social disruption caused by flood hazards.</i>	See response to Land Use Goal 17.
<b>Policy 3.4</b>	<i>Ensure that development does not divert storm water run off onto adjacent properties, or cause alterations of natural drainage courses that cannot be adequately handled by existing drainage facilities or the flood control improvements proposed with the development.</i>	The project will implement a drainage system that will convey existing storm water around the development footprint and will be consistent with existing drainage patterns. Further, the project will mitigate storm water runoff to a flow rate equivalent to the pre-developed condition. See response to Land Use Goal 17.
<b>Policy 3.5</b>	<i>Require the installation and maintenance of storm drains by property owners.</i>	See response to Land Use Goal 17.

**APPENDIX B**

**Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
<b>Safety/Risk Management</b>		
<b>Goal 4</b>	<i>Reduce threats to public safety and protect property from wildland and urban fire hazards.</i>	According to the Map My County – Riverside County database (County of Riverside 2015), the project is not within a designated fire hazard area.
<b>Policy 4.5</b>	<i>Ensure that new access roads have adequate widths and turning radius for fire and emergency vehicles.</i>	project roadways meet fire and emergency vehicle standards.
<b>Policy 4.7</b>	<i>Encourage the planting and maintenance of drought-resistant, fire-retardant species on slopes to reduce the risk of brush fire and soil erosion in areas adjacent to hillsides; and develop stringent site design and maintenance standards for areas with high fire hazard.</i>	According to the Map My County – Riverside County database (County of Riverside 2015), the project is not within a designated fire hazard area. In addition, the project will be constructed in a relatively flat area and will not impact hillsides. Furthermore, the project will comply with the Specific Plan Design Guidelines, which require the use of drought-resistant landscaping by the project.
<b>Goal 5</b>	<i>Reduce the potential for hazardous material exposure or contamination in the Planning Area.</i>	The transport of all hazardous materials, is regulated by the U.S. Department of Transportation (Title 49 of the Code of Federal Regulations, the California Highway Patrol (Title 13 of the California Code of Regulations), and the California State Fire Marshall (Title 19 of the California Code of Regulations). In addition, in order to operate in the State of California, all hazardous materials transporters must be registered with the California Department of Toxic Substances Control (DTSC). These regulations minimize the potential for incidents involving hazardous materials.
<b>Policy 5.1</b>	<i>Comply with the enforcement of disclosure laws that require all users, producers, and transporters of hazardous materials and wastes to clearly identify such materials at the site, and to notify the appropriate County, State and/or Federal agencies in the event of a violation.</i>	See response to Safety Risk/Management Goal 5.

**APPENDIX B**

**Veteran's Industrial Park 215 Specific Plan - General Plan Consistency Table**

General Plan Goal/Policy		Consistency
<b>Safety/Risk Management</b>		
<b>Policy 5.3</b>	<i>Require land uses involved in the production, storage, transportation, handling, or disposal of hazardous materials are located a safe distance from land uses that may be adversely impacted by such activities.</i>	See response to Safety Risk/Management Goal 5.
<b>Policy 5.4</b>	<i>Ensure the storage, use and transportation of any hazardous materials complies with the standards set forth within the errata sheets published for each substance.</i>	See response to Safety Risk/Management Goal 5.
<b>Goal 7</b>	<i>Reduce the possible risk of upset, injury, and loss of life, property damage and other impacts associated with an aviation facility.</i>	The project is designed to incorporate appropriate uses as defined in the Air Installation Compatible Use Zone (AICUZ) Study, Airport Layout Plan, Department of Defense Instructions and Air Force Instructions. The project's land use plan and development regulations include height limitations associated with the 35-foot building restriction line and the 7:1 height requirements. In addition, the proposed uses comply with the Zone B occupancy and use restrictions. Security fencing will be provided adjacent to the runway. In addition, no standing water exceeding the airport's criteria will be allowed as part of the project's drainage plans.
<b>Policy 7.1</b>	<i>Ensure development and use of property within the vicinity of airfield complies with appropriate building standards and codes including height restrictions, restrictions on use, setbacks, population densities, insulation and materials, as contained within an approved Comprehensive Land Use Plan (CLUP) and appropriate AICUZ.</i>	See response to Safety Risk/Management Goal 7.

**March Air Reserve Base /  
Inland Port Airport  
Land Use Compatibility Plan**

**Adopted by**

**Riverside County Airport Land Use Commission**

**November 13, 2014**

**Prepared by**



**Santa Rosa, California**

# MARCH AIR RESERVE BASE / INLAND PORT AIRPORT LAND USE COMPATIBILITY PLAN

Adopted November 13, 2014

## OVERVIEW

This *March Air Reserve Base / Inland Port Airport Land Use Compatibility Plan (March ARB/IPA ALUCP)* was prepared for and adopted by the Riverside County Airport Land Use Commission (RCALUC). In accordance with provisions of the California State Aeronautics Act (Public Utilities Code Section 21670 *et seq.*), the RCALUC has been assigned the lead responsibility for airport land use compatibility planning around each of the public-use and military airports in Riverside County, including the preparation of an ALUCP for each airport.

Beginning in 2004, the RCALUC began adopting new versions of the ALUCPs for most of these airports. Each of these individual ALUCPs is contained within a single, countywide document entitled *Riverside County Airport Land Use Compatibility Plan*. The ALUCP for each airport consists of the policies in Chapter 2 of that document that are applicable to all of the airports in the county together with airport-specific policies and maps in Chapter 3. This material plus an introductory chapter (Chapter 1) and a set of appendices comprise Volume I. Background data regarding each airport and its environs is included in Volumes 2 and 3.

This *March ARB/IPA ALUCP* maintains this established format. Thus, only the policies and maps specific to March ARB/IPA for insertion into Chapter 3 and the background data to be added to Volume 2 are presented here. All of the countywide policies in Chapter 2 of Volume 1 are considered to be part of the *March ARB/IPA ALUCP* unless explicitly modified or supplemented by the March-specific policies. The introductory and appendix content is also applicable although no ALUC policy is included therein.

*Insert for Riverside County ALUCP, Volume 1, Chapter 3, Individual Airport Policies and Compatibility Maps*

## MA. MARCH AIR RESERVE BASE/INLAND PORT AIRPORT

### MA.1 Compatibility Map Delineation

- 1.1 *Airport Master Plan Status:* The *Compatibility Plan* for March ARB/IPA is primarily based upon the U.S. Air Force's *Air Installation Compatibility Use Zones Study* for March Air Reserve Base (AICUZ) dated August 2005. Noise contours included in the AICUZ have been supplemented by more recent contours prepared for the Air Force and March Joint Powers Authority. These contours reflect current and projected fleet mix changes as indicated in Policy MA.1.3 below. The compatibility zones and associated criteria set forth in the *March ARB/IPA Compatibility Plan* provide noise and safety compatibility protection equivalent to or greater than the Air Force recommended criteria presented in the AICUZ.
- 1.2 *Airfield Configuration:* The airfield consists of two runways. The primary runway (Runway 14-32)—oriented north-northwest/south-southwest—is 13,300 feet in length and is the longest runway open to civilian use in the state. The second smaller runway, Runway 12-30, is just over 3,000 feet; its use is and will continue to be restricted to military-related light aircraft (primarily Aero Club activity). The airport has straight-in instrument approach capabilities to Runway 32 and a non-precision approach to Runway 14. No changes in the existing configuration of the airport runways and approaches are anticipated.
- 1.3 *Airport Activity:* The *Compatibility Plan* reflects a composite of potential future military and civilian aircraft activity scenarios (see discussion in Chapter W7). The data primarily relied upon for future mission military activity is as indicated in the 2013 environmental study analyzing the impacts of a fleet mix conversion from F-16 to F-15 fighter aircraft [*F-15 Aircraft Conversion Environmental Impact Statement 144th Fighter Wing California Air National Guard Fresno-Yosemite International Airport* (National Guard Bureau, March 2013)]. This study indicates potential maximum mission activity as 54,104 annual operations by military transport, tanker, fighter, and helicopter aircraft, together with military contract air carrier and military Aero Club aircraft. Additionally, for the purposes of assessing land use noise compatibility, noise impacts reflected in three other studies are taken into account in the compatibility zones shown on Map MA-1, Compatibility Map, of this chapter: the 2005 AICUZ Study [*Air Installation Compatible Use Zone Study for March Air Reserve Base (AICUZ)* (Department of the Air Force, August 2005)]; the Total Force Integration study [*Environmental Assessment for Proposed Military Construction and Total Force Integration at March Air Reserve Base* (Air Force Reserve Command, June 2010)]; and a study of general aviation facility needs done for the March Joint Powers Authority [*Environmental Impact Report for March Inland Port General Aviation Facilities Development* (March Joint Powers Authority, August 2012)]. Future maximum civilian aircraft activity is limited by the joint use agreement and related air quality conformity determination to 21,000 annual operations. While the number of future aircraft operations indicated in each of these studies is similar, the mix of aircraft types and other factors that affect noise impacts differ.
- 1.4 *Airport Influence Area:* The factors used in defining the airport influence area for March ARB/IP and the individual compatibility zones within the airport influence area are



indicated in Table MA-1. Table 3A which is applicable to other airports in the county does not apply to March ARB/IP. Table MA-1 makes adjustments to Table 3A that take into account the comparatively large geographic extent of the airport's impacts. Also, Compatibility Zone C is divided into two separate zones, C1 and C2.

The outer limits of *Zone E* and the areas within the *High Terrain Zone* define the airport influence area for March ARB/IPA. On the east side of the airfield, Zone E is established at 14,000 feet from the runway centerline. This distance is equivalent to the outer limits of the civilian airport conical surface, as established by FAR Part 77. The compatibility zones on the west side of the airport are more extensive because those areas are routinely overflowed by both military and civilian aircraft.

## MA.2 Additional/Specific Compatibility Policies

Policies set forth in Chapter 2, Countywide Policies, shall be modified or supplemented for the *March ARB/IPA ALUCP* as follows.

### 2.1 *Basic Land Use Compatibility Criteria:*

- (a) Countywide Table 2A: The basic compatibility criteria listed in Table 2A do not apply to the environs of March ARB/IPA. The compatibility criteria that shall be applicable to the March ARB/IPA influence area are set forth in Table MA-2. For the purposes of land use compatibility matters involving the March ARB/IPA influence area, any reference to Table 2A in the policies of Chapter 2 shall instead be taken as a reference to Table MA-2.
- (b) Countywide Policy 3.1.3(b): The policy concerning residential densities in Compatibility Zone D is not applicable to March ARB/IPA.
- (c) Countywide Policy 3.1.4(b): The reference to special risk-reduction building design measures is not applicable to March ARB/IPA.

2.2 *Infill:* Countywide Policy 3.3.1(a)(2) notwithstanding, infill residential development in the vicinity of March ARB/IPA need only be 50% bounded by similar uses to qualify as infill. All other provisions of Countywide Policy 3.3.1 apply.

### 2.3 *Supporting Compatibility Criteria for Noise:*

- (a) Countywide Policy 4.1.5: The CNEL considered normally acceptable for new residential land uses in the vicinity of March ARB/IPA is 65 dB. Table 2B is not applicable.
- (b) Countywide Policy 4.1.6: Single-event noise levels from aircraft operations can be particularly intrusive at night. Compared to other airports in the county, current and projected nighttime activity by large aircraft at March ARB/IPA warrants a greater degree of sound attenuation for the interiors of buildings housing certain uses as cited below.
  - (1) The maximum, aircraft-related, interior noise level that shall be considered acceptable shall be CNEL 40 dB for all new residences, schools, libraries, museums, hotels and motels, hospitals and nursing homes, places of worship, and other noise-sensitive uses. For office uses, the interior standard shall be CNEL 45 dB, the same as the countywide criterion.

Zone	Noise and Overflight Factors	Safety and Airspace Protection Factors
<b>M</b> <i>(Military)</i>	<b>Federal Lands</b> <ul style="list-style-type: none"> <li>▶ No ALUC authority</li> </ul>	<b>Federal Lands</b> <ul style="list-style-type: none"> <li>▶ No ALUC authority</li> </ul>
<b>A</b> <i>Clear Zone</i> <i>(if not on base)</i>	<b>Noise Impact: Very High</b> <ul style="list-style-type: none"> <li>▶ High CNEL and single-event noise levels</li> </ul>	<b>Risk Level: Very High</b> <ul style="list-style-type: none"> <li>▶ Dimensions set to include Clear Zone as indicated in Air Installation Compatible Use Zone (AICUZ) study for airport</li> <li>▶ Generally on air base property or controlled by easements</li> </ul>
<b>B1</b> <i>Inner Approach/Departure Zone</i>	<b>Noise Impact: High</b> <ul style="list-style-type: none"> <li>▶ Within or near 85-CNEL contour</li> <li>▶ Single-event noise sufficient to disrupt many land use activities including indoors if windows open</li> </ul>	<b>Risk Level: High</b> <ul style="list-style-type: none"> <li>▶ Within Accident Potential Zone I or II</li> <li>▶ Additionally, zone boundary to north reflects turning flight tracks</li> </ul>
<b>B2</b> <i>High Noise Zone</i>	<b>Noise Impact: High</b> <ul style="list-style-type: none"> <li>▶ Within or near 85-CNEL contour</li> <li>▶ Single-event noise sufficient to disrupt many land use activities including indoors if windows open</li> </ul>	<b>Risk Level: Moderate</b> <ul style="list-style-type: none"> <li>▶ Beneath or adjacent to final approach and initial departure flight corridors or adjacent to runway</li> <li>▶ Not within Accident Potential Zones</li> </ul>
<b>C1</b> <i>Primary Approach/Departure Zone</i>	<b>Noise Impact: Moderate to High</b> <ul style="list-style-type: none"> <li>▶ Within or near 80-CNEL contour</li> <li>▶ Single-event noise may be disruptive to noise-sensitive land use activities; aircraft &lt;2,000 feet above runway elevation on arrival and generally &lt;3,000 feet above runway elevation on departure</li> </ul>	<b>Risk Level: Moderate</b> <ul style="list-style-type: none"> <li>▶ Beneath or adjacent to low altitude overflight corridors</li> </ul>
<b>C2</b> <i>Flight Corridor Zone</i>	<b>Noise Impact: Moderate</b> <ul style="list-style-type: none"> <li>▶ Within 80 CNEL contour, but more than 5 miles from runway end; or</li> <li>▶ Outside 80-CNEL contour, but regularly overflown in mostly daytime flight training</li> <li>▶ Single-event noise may be disruptive to noise-sensitive land use activities; aircraft &lt;3,000 feet above runway elevation on arrival</li> </ul>	<b>Risk Level: Moderate to Low</b> <ul style="list-style-type: none"> <li>▶ Distant (beyond 5 miles) portion of Instrument arrival corridor; or</li> <li>▶ Closed-circuit flight training activity corridors</li> </ul>
<b>D</b> <i>Flight Corridor Buffer</i>	<b>Noise Impact: Moderate to Low</b> <ul style="list-style-type: none"> <li>▶ Mostly within 55-CNEL contour</li> <li>▶ More concern with respect to individual loud events than with cumulative noise contours</li> </ul>	<b>Risk Level: Low</b> <ul style="list-style-type: none"> <li>▶ On periphery of flight corridors</li> <li>▶ Risk concern primarily with uses for which potential consequences are severe (e.g. very-high-intensity activities in a confined area)</li> </ul>
<b>E</b> <i>Other Airport Environs</i>	<b>Noise Impact: Low</b> <ul style="list-style-type: none"> <li>▶ Beyond 55-CNEL contour</li> <li>▶ Occasional overflights intrusive to some outdoor activities</li> </ul>	<b>Risk Level: Low</b> <ul style="list-style-type: none"> <li>▶ Within outer or occasionally used portions of flight corridors</li> </ul>
<b>*</b> <i>High Terrain Zone</i>	<b>Noise Impact: Low</b> <ul style="list-style-type: none"> <li>▶ Individual noise events slightly louder because high terrain reduces altitude of overflights</li> </ul>	<b>Risk Level: Moderate</b> <ul style="list-style-type: none"> <li>▶ Moderate risk because high terrain constitutes airspace obstruction</li> <li>▶ Concern is tall single objects (e.g., antennas)</li> </ul>

Table MA-1

## Compatibility Zone Factors

### March Air Reserve Base / Inland Port Airport

- (2) To ensure compliance with these criteria, an acoustical study shall be required to be completed for any development proposed to be situated where the aviation-related noise exposure is more than 20 dB above the interior standard (e.g., within the CNEL 60 dB contour where the interior standard is CNEL 40 dB). Standard building construction is presumed to provide adequate sound attenuation where the difference between the exterior noise exposure and the interior standard is 20 dB or less.

#### 2.4 *Supporting Compatibility Criteria for Safety:*

- (a) Countywide Policy 4.2.3: The acceptability of land uses of special concern within certain compatibility zones around March ARB/IPA shall be evaluated in accordance with the criteria indicated in Table MA-2. The criteria listed in Countywide Policy 4.2.3 do not apply.
- (b) Countywide Policy 4.2.4: The requirements for open land do not apply to the vicinity of March ARB/IPA except with regard to Compatibility Zones A and B1.
- (c) Countywide Policy 4.2.5: For the vicinity of March ARB/IPA, new nonresidential development shall not be clustered in a manner that would result in a usage intensity within any one acre (the number of people per single acre) exceeding the limits specified in Table MA-2. Clustering of residential development is encouraged, but the density within any one acre shall be limited to no more than 4.0 times the allowable average density for the zone in which the development is proposed.
- (d) Countywide Policy 4.2.6: The policy concerning risk reduction through building design is not applicable to the March ARB/IPA influence area.
- (e) Calculation of Usage Intensities for Retail Uses: Notwithstanding the provisions of Appendix C and Table C1 of the *Riverside County Airport Land Use Compatibility Plan*, the usage intensities of retail sales and display areas (a.k.a. mercantile areas) or “showrooms” (excluding restaurants and other uses specifically identified separately from retail/mercantile in Table C1) shall be evaluated as having an occupancy level of 115 gross square feet per person without eligibility for the 50 percent reduction in the resulting usage intensity (people per acre) as described in the appendix.
- (f) Calculation of Usage Intensities for Warehouse Uses: Notwithstanding the provisions of Appendix C and Table C1 of the *Riverside County Airport Land Use Compatibility Plan*, the usage intensities of warehouses, distribution centers, e-commerce centers, fulfillment centers, and similar uses in buildings larger than 200,000 gross square feet, exclusive of offices, conference rooms, break rooms and other uses identified separately from warehouses in Table C1, shall be calculated as follows:
- (1) High-cube warehouses and distribution centers, other than e-commerce centers and fulfillment centers, shall be evaluated on the basis of 35% of the usage intensity that results from the occupancy level indicated in Table C1.
  - (2) E-commerce centers, fulfillment centers, and other similar uses shall be evaluated on the basis of 50% of the usage intensity that results from the occupancy level indicated in Table C1.

- (3) Office space in these buildings shall be evaluated on the basis of 50% of the usage intensity that results from the occupancy level indicated in Table C1. All other separately identified uses shall be evaluated on the basis of the occupancy level listed for the respective use in Table C1.

2.5 *Supporting Compatibility Criteria for Airspace Protection:*

- (a) *Countywide Policy 4.3.3:* For proposed objects in the March ARB/IPA vicinity, the heights requiring ALUC review shall be as specified in Table MA-2.
- (b) *Countywide Policy 4.3.4:* Heights of objects shall be restricted in accordance with the airspace protection surfaces depicted in Table MA-2.
- (c) *Countywide Policy 4.3.5:* The compatibility zones within which dedication of an aviation easement shall be required as a condition of development is as indicated in Table MA-2. Except within Compatibility Zone A, aviation easements shall be dedicated to the March Inland Port Airport Authority or other civilian agency that may supersede it (successor-in-interest). Any aviation easements required within Zone A shall be dedicated to the United States of America.
- (d) *Countywide Policy 4.3.7:* Additional hazards to flight as listed in Table MA-2 are to be avoided in the vicinity of March ARB/IPA.

2.6 *Supporting Compatibility Criteria for Overflight:*

- (a) *Countywide Policy 4.4.3:* The compatibility zones within which a deed notice shall be required as a condition of development are as indicated in Table MA-2.

2.7 *Site-Specific Exceptions:*

Four development projects near March ARB have received entitlements in the form of Development Agreements or Disposition and Development Agreements from the respective jurisdictions prior to adoption of the *ALUCP* by the Riverside County ALUC. As such, exceptions to the compatibility criteria outlined in the preceding subsections are granted for these projects provided that they meet the conditions indicated below. (The locations of these exceptions are shown on Map MA-1 and the numbers below correspond to the numbering on that map.)

Exceptions for Sites 1 through 4 are valid only as long as the indicated specific plans and associated development agreements remain in effect. Any changes to the specific plans must be reviewed by the ALUC to ensure that increases in intensity of the proposed development would not result from the changes. Further, if the development agreements should expire, the criteria applicable to the property for which these exceptions apply shall revert to the underlying compatibility criteria indicated in this *ALUCP*.

- (a) *(Exception Site 1) March Business Center Specific Plan (SP-1) and Meridian (SP-5), March Joint Powers Authority*
- (1) Situated in Compatibility Zones B1, B2, C1, C2 and D.
- (2) March Business Center, a 1,032-acre, non-residential business park located at the southwest corner of Alessandro Boulevard and I-215 freeway within the March Joint Powers Authority, approved with specific airport compatibility provisions

(Ord. #JPA 03-01, SP-1), subject to March JPA Resolution #JPA 11-17 limiting development within the Accident Potential Zones and vested through a development Agreement recorded on June 7, 2004.

- (3) Meridian, a 258-acre portion of the original March Business Center, consisting of a nonresidential business park located at the southwest corner of Alessandro Boulevard and I-215 freeway within the March Joint Powers Authority, approved with specific airport compatibility provisions (Ord. #JPA 10-02, SP-5), subject to March JPA Resolution #JPA 11-17 limiting development within the Accident Potential Zones and vested through a development Agreement recorded on June 7, 2004.
  - (4) For the purpose of this *Compatibility Plan*, the Meridian exception area specifically allows development of a hotel or hotels on the 13-acre site situated within Compatibility Zone B2 and bordered by Interstate 215 on the east and Van Buren Boulevard on the south. Any such hotel or hotels shall be limited as follows: maximum of 100 people per acre; maximum of 250 people per single acre; maximum of 3 above-ground habitable floors; no conference facilities (however, small meeting room(s) for a total of up to 50 people is (are) acceptable). Sound attenuation as appropriate for the combined airport and freeway noise levels shall be provided.
  - (5) The Development Agreement referenced in Paragraphs (2) and (3) above expires on December 27, 2016. After that, the agreement provides for two more 5-year automatic extensions. The developer must request the Development Agreement extensions and the Authority must make findings that the development is still in substantial conformance.
- (b) *(Exception Site 2) Harvest Landing Specific Plan, City of Perris*
- (1) Situated in Compatibility Zone C2.
  - (2) A 341-acre mixed-use Specific Plan located south of Placentia Avenue and east of Interstate 215 within the City of Perris and authorizing 1,860 residential units and 1,306,582 square feet of business/commercial uses. The Specific Plan and associated Development Agreement were adopted in May 2011.
  - (3) Agreement will expire 15 years from the approval date plus extensions in 5-year increments subject to City Council approval.
- (c) *(Exception Site 3) Park West Specific Plan, City of Perris*
- (1) Situated in Compatibility Zones C1 and C2.
  - (2) A 534.3-acre residential Specific Plan located south of Nuevo Rd and east of the Perris Valley Storm Channel within the City of Perris and authorized for a maximum of 2,027 residential units as identified in the Specific Plan and Development Agreement approved by Council on January 30, 2007.
  - (3) Agreement for Phase I expires 10 years from the approval date. Phases II and III extend the agreement to 2027 or 10 years after the developer submits an application for approval of a tentative tract map for any portion of these phases.

(d) *(Exception Site 4) Day/Alessandro Affordable Housing Site, City of Moreno Valley*

- (1) Situated in Compatibility Zone C1.
- (2) A planned 8.43-acre multifamily site located at the northeast corner of Day Street and Alessandro Boulevard within the City of Moreno Valley approved as a maximum 225 unit multifamily development through an existing Disposition and Development Agreement approved on May 26, 2009.
- (3) The city owns the site, thus an expiration date is not applicable.

(e) *(Exception Site 5) Ben Clark Training Center*

- (1) Situated in Compatibility Zones C2 and D. This site specific exception is applicable to the portion of the property located within Zone C2.
- (2) An approximately 375-acre property located within unincorporated Riverside County deeded to the County by the U.S. Department of Defense as part of the 1996 instrument of transfer. Provisions of the transfer explicitly restrict use of the property to training of law enforcement and public safety personnel.
- (3) Notwithstanding the criteria set forth in Table MA-2, the following provisions shall apply to future development of the portions of Ben Clark Training Center situated within Compatibility Zone C2:
  - Future development of the property shall be consistent with the deed restrictions.
  - Any overnight occupancy of facilities must pertain to and be in furtherance of the function and purpose of the property as dictated by the property's deed restrictions.
  - Use of part of the property as an educational facility operated by the Riverside Community College District, Moreno Valley College, is permitted and not considered to be a "general college" provided that this use continues to be related to law enforcement and public safety training purposes.
  - Use of the property shall adhere to the average-acre intensity limit of 200 people per acre as established in Table MA-2. However, the single-acre intensity limit of Table MA-2 shall not apply.
  - New buildings shall be restricted to three (3) floors except that training towers or similar structures used specifically for the purpose of training law enforcement and public safety personnel may exceed this limit.
  - All other requirements applicable to Zone C2 as set forth in Table MA-2 shall continue to apply, including those pertaining to airspace review, electromagnetic radiation notification, and deed notice and disclosure.

(f) *(Exception Site 6) Ridge Crest Cardinal Subdivision, City of Riverside*

- (1) Situated in Compatibility Zone C2.

- (2) A 13.54-acre proposed single-family residential subdivision located east of Trautwein Road and north of Grove Community Avenue within the City of Riverside.
- (3) Notwithstanding the criteria set forth in Table MA-2, the following provisions shall apply to future development of this property:
  - An average-acre density of up to 6.5 dwelling units per acre (a maximum of 87 dwelling units) shall be allowed in lieu of the 6.0 dwelling units per acre set by Table MA-2.
  - Exception Site 6 is a portion of an area covered by a Development Agreement between the City of Riverside and The Grove Community Church recorded on November 26, 2003 as Instrument No. 2003-934365. The Development Agreement provided for a senior housing facility, elementary school, and preschool within the area where the Ridge Crest Cardinal subdivision is now proposed. Development of the proposed single-family residential subdivision would utilize the area previously proposed for these facilities and thereby reduce the potential number of vulnerable occupants at this location, in comparison to these entitled but unbuilt uses. The above allowance for up to 6.5 dwelling units per acre on the property is only applicable if these previously entitled uses are not constructed within the boundaries of Exception Site 6.

Zone	Locations	Density / Intensity Standards			Additional Criteria		
		Residential (d.u./ac) <sup>1</sup>	Other Uses (people/ac) <sup>2</sup> Average <sup>5</sup> Single Acre <sup>6</sup>	Req'd Open Land	Prohibited Uses <sup>3</sup>	Other Development Conditions <sup>4</sup>	
<b>M</b>	Military				> No ALUC authority		
<b>A</b>	Clear Zone <sup>7</sup>	No new dwellings allowed	0	0	All Remaining	<ul style="list-style-type: none"> <li>&gt; All non-aeronautical structures</li> <li>&gt; Assemblages of people</li> <li>&gt; Objects exceeding FAR Part 77 height limits</li> <li>&gt; All storage of hazardous materials</li> <li>&gt; Hazards to flight <sup>8</sup></li> </ul>	<ul style="list-style-type: none"> <li>&gt; Electromagnetic radiation notification <sup>9</sup></li> <li>&gt; Avigation easement dedication and disclosure <sup>4,7</sup></li> </ul>
<b>B1</b>	Inner Approach/Departure Zone	No new dwellings allowed <sup>10</sup>	25 (APZ I) 50 (APZ II and outside APZs) <sup>11</sup>	100 100	Max. 50% lot coverage within APZs <sup>12</sup>	<ul style="list-style-type: none"> <li>&gt; Children's schools, day care centers, libraries</li> <li>&gt; Hospitals, congregate care facilities, hotels/motels, restaurants, places of assembly</li> <li>&gt; Bldgs with &gt; 1 aboveground habitable floor in APZ I or &gt; 2 floors in APZ II and outside of APZs <sup>13</sup></li> <li>&gt; Hazardous materials manufacture/storage <sup>14</sup></li> <li>&gt; Noise sensitive outdoor nonresidential uses <sup>15</sup></li> <li>&gt; Critical community infrastructure facilities <sup>16</sup></li> <li>&gt; Hazards to flight <sup>8</sup></li> <li>&gt; Uses listed in AICUZ as not compatible in APZ I or APZ II <sup>17</sup></li> </ul>	<ul style="list-style-type: none"> <li>&gt; Locate structures maximum distance from extended runway centerline</li> <li>&gt; Sound attenuation as necessary to meet interior noise level criteria <sup>18</sup></li> <li>&gt; Zoned fire sprinkler systems required</li> <li>&gt; Airspace review req'd for objects &gt; 35 ft. tall <sup>19</sup></li> <li>&gt; Electromagnetic radiation notification <sup>9</sup></li> <li>&gt; Avigation easement dedication and disclosure <sup>4</sup></li> </ul>
<b>B2</b>	High Noise Zone	No new dwellings allowed <sup>10</sup>	100	250	No Req't	<ul style="list-style-type: none"> <li>&gt; Children's schools, day care centers, libraries</li> <li>&gt; Hospitals, congregate care facilities, hotels/motels, places of assembly</li> <li>&gt; Bldgs with &gt; 3 aboveground habitable floors</li> <li>&gt; Noise-sensitive outdoor nonresidential uses <sup>15</sup></li> <li>&gt; Critical community infrastructure facilities <sup>16</sup></li> <li>&gt; Hazards to flight <sup>8</sup></li> </ul>	<ul style="list-style-type: none"> <li>&gt; Locate structures max. distance from runway</li> <li>&gt; Sound attenuation as necessary to meet interior noise level criteria <sup>18</sup></li> <li>&gt; Aboveground bulk storage of hazardous materials discouraged <sup>14,20</sup></li> <li>&gt; Airspace review req'd for objects &gt; 35 ft. tall <sup>19</sup></li> <li>&gt; Electromagnetic radiation notification <sup>9</sup></li> <li>&gt; Avigation easement dedication and disclosure <sup>4</sup></li> </ul>
<b>C1</b>	Primary Approach/Departure Zone	≤ 3.0	100	250	No Req't	<ul style="list-style-type: none"> <li>&gt; Children's schools, day care centers, libraries</li> <li>&gt; Hospitals, congregate care facilities, places of assembly</li> <li>&gt; Noise-sensitive outdoor nonresidential uses <sup>15</sup></li> <li>&gt; Hazards to flight <sup>8</sup></li> </ul>	<ul style="list-style-type: none"> <li>&gt; Critical community infrastructure facilities discouraged <sup>14,20</sup></li> <li>&gt; Aboveground bulk storage of hazardous materials discouraged <sup>14,20</sup></li> <li>&gt; Sound attenuation as necessary to meet interior noise level criteria <sup>18</sup></li> <li>&gt; Airspace review req'd for objects &gt; 70 ft. tall <sup>19</sup></li> <li>&gt; Electromagnetic radiation notification <sup>9</sup></li> <li>&gt; Deed notice and disclosure <sup>4</sup></li> </ul>
<b>C2</b>	Flight Corridor Zone	≤ 6.0	200	500	No Req't	<ul style="list-style-type: none"> <li>&gt; Highly noise-sensitive outdoor nonresidential uses <sup>15</sup></li> <li>&gt; Hazards to flight <sup>8</sup></li> </ul>	<ul style="list-style-type: none"> <li>&gt; Children's schools discouraged <sup>20</sup></li> <li>&gt; Airspace review req'd for objects &gt; 70 ft. tall <sup>19</sup></li> <li>&gt; Electromagnetic radiation notification <sup>9</sup></li> <li>&gt; Deed notice and disclosure <sup>4</sup></li> </ul>
<b>D</b>	Flight Corridor Buffer	No Limit	No restriction <sup>21</sup>	No Req't	No Req't	<ul style="list-style-type: none"> <li>&gt; Hazards to flight <sup>8</sup></li> </ul>	<ul style="list-style-type: none"> <li>&gt; Major spectator-oriented sports stadium, amphitheaters, concert halls discouraged <sup>21</sup></li> <li>&gt; Electromagnetic radiation notification <sup>9</sup></li> <li>&gt; Deed notice and disclosure <sup>4</sup></li> </ul>
<b>E</b>	Other Airport Environs	No Limit	No Restriction <sup>21</sup>	No Req't	No Req't	<ul style="list-style-type: none"> <li>&gt; Hazards to flight <sup>8</sup></li> </ul>	<ul style="list-style-type: none"> <li>&gt; Disclosure only <sup>4</sup></li> </ul>
<b>F</b>	High Terrain	Same as Underlying Compatibility Zone			Not Applicable	<ul style="list-style-type: none"> <li>&gt; Hazards to flight <sup>8</sup></li> <li>&gt; Other uses restricted in accordance with criteria for underlying zone</li> </ul>	<ul style="list-style-type: none"> <li>&gt; Airspace review req'd for objects &gt; 35 ft. tall <sup>19</sup></li> <li>&gt; Avigation easement dedication and disclosure <sup>4</sup></li> </ul>

Table MA-2

## Basic Compatibility Criteria

March Air Reserve Base / Inland Port Airport



## NOTES:

Policies referenced here are from the *Riverside County Airport Land Use Compatibility Plan* adopted by the Riverside County ALUC for other airports beginning in October 2004. The countywide policies are hereby incorporated into the *March ARB/PA ALUCP* except as modified or supplemented by the policies in Section MA.2 of this chapter. A complete copy of the *Riverside County Airport Land Use Compatibility Plan* is available on the Riverside County Airport Land Use Commission website at [www.rcaluc.org](http://www.rcaluc.org).

- <sup>1</sup> Residential development must not contain more than the indicated number of dwelling units (excluding secondary units) per gross acre. Clustering of units is encouraged provided that the density is limited to no more than 4.0 times the allowable average density for the zone in which the development is proposed. Gross acreage includes the property at issue plus a share of adjacent roads and any adjacent, permanently dedicated, open lands. Mixed-use development in which residential uses are proposed to be located in conjunction with nonresidential uses in the same or adjoining buildings on the same site shall be treated as nonresidential development for the purposes of usage intensity calculations; that is, the occupants of the residential component must be included in calculating the overall number of occupants on the site. A residential component shall not be permitted as part of a mixed use development in zones where residential uses are indicated as incompatible. See Countywide Policy 3.1.3(d). All existing residential development, regardless of densities, is not subject to ALUC authority.
- <sup>2</sup> Usage intensity calculations shall include all people (e.g., employees, customers/visitors, etc.) who may be on the property at a single point in time, whether indoors or outside.
- <sup>3</sup> The uses listed here are ones that are explicitly prohibited regardless of whether they meet the intensity criteria. In addition to these explicitly prohibited uses, other uses will normally not be permitted in the respective compatibility zones because they do not meet the usage intensity criteria. See *Riverside County Airport Land Use Compatibility Plan*, Volume 1, Appendix D for a full list of compatibility designations for specific land uses.
- <sup>4</sup> As part of certain real estate transactions involving residential property within any compatibility zone (that is, anywhere within an airport influence area), information regarding airport proximity and the existence of aircraft overflights must be disclosed. This requirement is set by state law. See Countywide Policy 4.4.2 for details. Easement dedication and deed notice requirements indicated for specific compatibility zones apply only to new development and to reuse if discretionary approval is required. Except within Zone A (Clear Zone), aviation easements are to be dedicated to the March Inland Port Airport Authority. See sample language in [www.marchipa.com/docs/forms/aviationeasement.pdf](http://www.marchipa.com/docs/forms/aviationeasement.pdf). Any aviation easements required within Zone A shall be dedicated to the United States of America.
- <sup>5</sup> The total number of people permitted on a project site at any time, except rare special events, must not exceed the indicated usage intensity times the gross acreage of the site. Rare special events are ones (such as an air show at the airport) for which a facility is not designed and normally not used and for which extra safety precautions can be taken as appropriate.
- <sup>6</sup> Clustering of nonresidential development is permitted. However, no single acre of a project site shall exceed the indicated number of people per acre. See Countywide Policy 4.2.5 for details.
- <sup>7</sup> Clear zone (equivalent to runway protection zone at civilian airports) limits that delineate Zone A are derived from locations indicated in the March Air Reserve Base AICUZ study. See Note 4 for aviation easement dedication requirements in this zone.
- <sup>8</sup> Hazards to flight include physical (e.g., tall objects), visual, and electronic forms of interference with the safety of aircraft operations. Land use development that may cause the attraction of birds to increase is also prohibited. Man-made features must be designed to avoid heightened attraction of birds. In Zones A, B1, and B2, flood control facilities should be designed to hold water for no more than 48 hours following a storm and be completely dry between storms (see FAA Advisory Circular 150/5200-33B). Additionally, certain farm crops and farming practices that tend to attract birds are strongly discouraged. These include: certain crops (e.g., rice, barley, oats, wheat – particularly durum – corn, sunflower, clover, berries, cherries, grapes, and apples); farming activities (e.g., tilling and harvesting); confined livestock operations (i.e., feedlots, dairy operations, hog or chicken production facilities, or egg-laying operations); and various farming practices (e.g., livestock feed, water, and manure). Fish production (i.e., catfish, trout) conducted outside of fully enclosed buildings may require mitigation measures (e.g., netting of outdoor ponds, providing covered structures) to prevent bird attraction. Also see Countywide Policy 4.3.7.
- <sup>9</sup> March ARB must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include microwave transmission in conjunction with a cellular tower, radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers and other similar EMR emissions.
- <sup>10</sup> Other than in Zone A, construction of a single-family home, including a second unit as defined by state law, on a legal lot of record is exempted from this restriction where such use is permitted by local land use regulations. Interior noise level standards and aviation easement requirements for the compatibility zone in which the dwelling is to be located are to be applied.
- <sup>11</sup> Non-residential uses are limited to 25 people per gross acre in Accident Potential Zone (APZ) I and 50 people per acre in APZ II and elsewhere in Zone B1. Single-acre intensity limits are 100 people/acre throughout Zone B1.
- <sup>12</sup> In APZ I, any proposed development having more than 20% lot coverage must not provide on-site services to the public. Zoned fire sprinklers are required. Also, in APZ I, site design of proposed development should to the extent possible avoid placement of buildings within 100 feet of the ex-

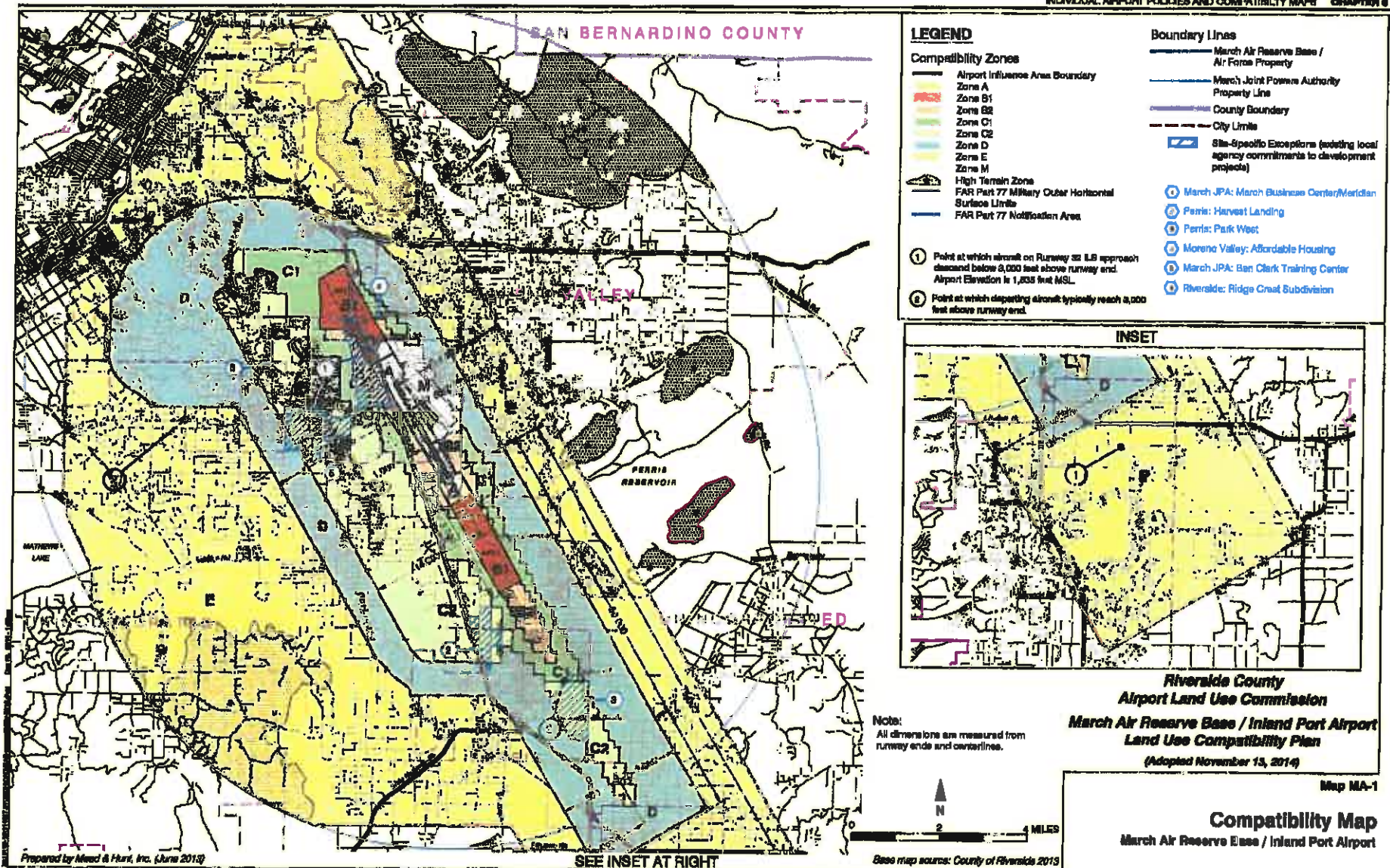
Table MA-2, continued

tended runway centerline; this center strip should be devoted to parking, landscaping, and outdoor storage. Maximum lot coverage is not limited outside the APZs.

- <sup>13</sup> Within APZ II and outside APZs, two-story buildings are allowed.
- <sup>14</sup> Storage of aviation fuel and other aviation-related flammable materials on the airport is exempted from this criterion. In APZ I, manufacture or bulk storage of hazardous materials (toxic, explosive, corrosive) is prohibited unless storage is underground; small quantities of materials may be stored for use on site. In APZ II and elsewhere within Zone B1, aboveground storage of more than 6,000 gallons of nonaviation flammable materials per tank is prohibited. In Zones B2 and C1, aboveground storage of more than 6,000 gallons of hazardous or flammable materials per tank is discouraged.
- <sup>15</sup> Examples of noise-sensitive outdoor nonresidential uses that should be prohibited include major spectator-oriented sports stadiums, amphitheatres, concert halls and drive-in theaters. Caution should be exercised with respect to uses such as poultry farms and nature preserves.
- <sup>16</sup> Critical community facilities include power plants, electrical substations, and public communications facilities. See Countywide Policy 4.2.3(d).
- <sup>17</sup> For properties in either APZ I or II, any use listed as "N – not compatible" for that particular APZ in Table 3-1 of the 2005 *Air Installation Compatible Use Zone Study for March Air Reserve Base*. Beyond the boundaries of the APZs in Zone B1, such uses are discouraged, but not necessarily prohibited unless otherwise specified herein.
- <sup>18</sup> All new residences, schools, libraries, museums, hotels and motels, hospitals and nursing homes, places of worship, and other noise-sensitive uses must have sound attenuation features incorporated into the structures sufficient to reduce interior noise levels from exterior aviation-related sources to no more than CNEL 40 dB. This requirement is intended to reduce the disruptiveness of loud individual aircraft noise events upon uses in this zone and represents a higher standard than the CNEL 45 dB standard set by state and local regulations and countywide ALUC policy. Office space must have sound attenuation features sufficient to reduce the exterior aviation-related noise level to no more than CNEL 45 dB. To ensure compliance with these criteria, an acoustical study shall be required to be completed for any development proposed to be situated where the aviation-related noise exposure is more than 20 dB above the interior standard (e.g., within the CNEL 80 dB contour where the interior standard is CNEL 40 dB). Standard building construction is presumed to provide adequate sound attenuation where the difference between the exterior noise exposure and the interior standard is 20 dB or less.
- <sup>19</sup> This height criterion is for general guidance. Airspace review requirements are determined on a site-specific basis in accordance with Part 77 of the Federal Aviation Regulations. Shorter objects normally will not be airspace obstructions unless situated at a ground elevation well above that of the airport. Taller objects may be acceptable if determined not to be obstructions. The Federal Aviation Administration or California Department of Transportation Division of Aeronautics may require marking and/or lighting of certain objects. See Countywide Policies 4.3.4 and 4.3.6 for additional information.
- <sup>20</sup> Discouraged uses should generally not be permitted unless no feasible alternative is available.
- <sup>21</sup> Although no explicit upper limit on usage intensity is defined for Zone D and E, land uses of the types listed—uses that attract very high concentrations of people in confined areas—are discouraged in locations below or near the principal arrival and departure flight tracks.

Table MA-2, continued

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FERRIS RESERVOIR

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**LEGEND**

**Compatibility Zones**

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

**Boundary Lines**

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (pending local agency commitments to development projects)

- ① Point at which aircraft on Runway 32 ILS approach descend below 5,000 feet above runway end. Airport Elevation is 1,000 feet MSL.
- ② Point at which departing aircraft typically reach 4,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ban Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision

**INSET**



**Riverside County  
Airport Land Use Commission  
March Air Reserve Base / Inland Port Airport  
Land Use Compatibility Plan  
(Adopted November 13, 2014)**

Note:  
All dimensions are measured from  
runway ends and centerlines.

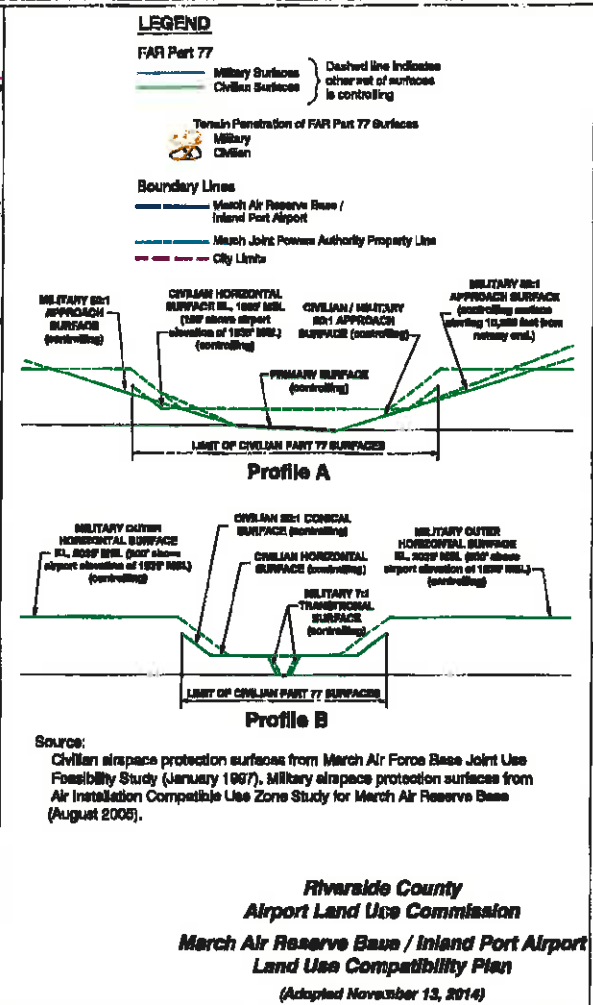
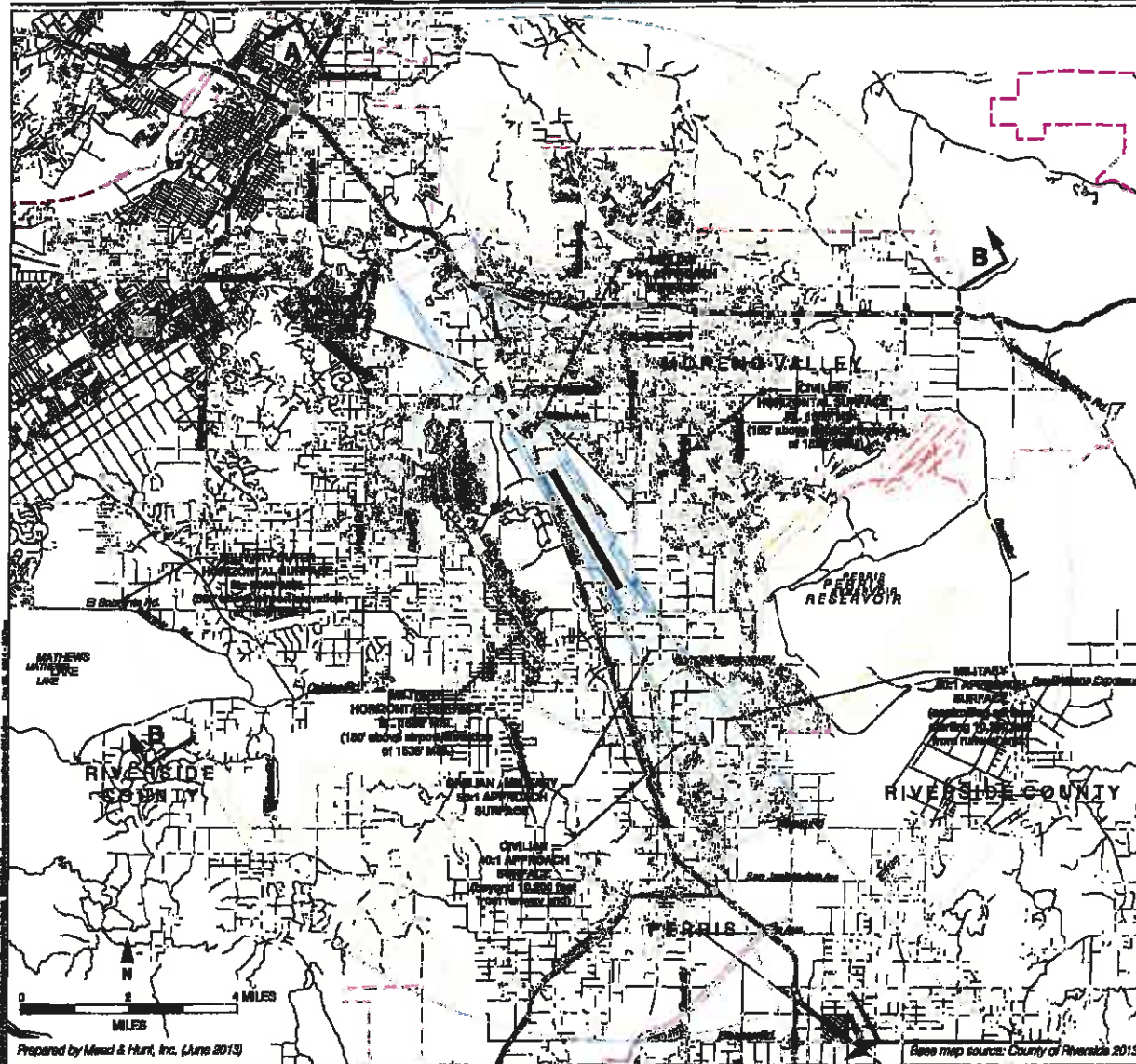


Map MA-1  
**Compatibility Map**  
March Air Reserve Base / Inland Port Airport

Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Base map source: County of Riverside 2013



Map MA-2

**Airspace Protection Surfaces  
March Air Reserve Base / Inland Port Airport**

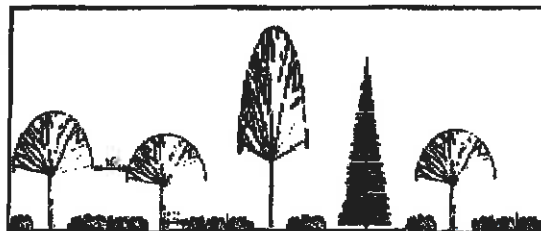


Figure 1. Selection of shrubs should be a mix of deciduous and coniferous species with no more than 50 percent evergreen species.

**Plant Selection, Irrigation, and Wildlife Management.** Riverside County requires landscaping for proposed development and redevelopment projects, and it is also committed to the use of native and drought-tolerant plants to reduce landscape-related water use. The County of Riverside Guide to California Friendly provides a lengthy plant palette to help landscape architects, planners, and the public select plant materials that will reduce water use in accordance with local and state goals: ([http://rcflma.org/Portals/7/documents/landscaping\\_guidelines/Guide\\_to\\_California\\_Friendly\\_Landscaping.pdf](http://rcflma.org/Portals/7/documents/landscaping_guidelines/Guide_to_California_Friendly_Landscaping.pdf).)

Many of the plants on the "County of Riverside California Friendly Plant List" could attract potentially hazardous wildlife species. Table 2 provides a reduced species list, nearly all of which were excerpted from the Friendly Plant List, but are less likely to support potentially hazardous wildlife. Project sponsors should use this list for projects within an AIA.

The list is not meant to be exhaustive, and other species may be appropriate based on the project location or other project-related circumstances. Sponsors who wish to propose plant materials that are not included in Table 1 will need to demonstrate to the ALUC that proposed species will be unlikely to attract hazardous wildlife to the AIA.

**General Guidelines.** Other factors can affect wildlife behavior. Landscaping can provide a food source, opportunities for shelter, nesting and perching. Proposed landscaping can help to discourage wildlife through the application of the following guidelines summarized below and described in Table 1.

- **Close the Restaurant!** Do not use plant material that produce a food source, such as edible fruit, seeds, berries, drupes, or palatable forage for grazing wildlife. When possible, select a non-fruiting variety or male cultivar.
- **No Vacancy!** Avoid densely branched or foliated trees; they provide ideal nesting habitat and shelter.
- **Prevent Littering!** Select tree species that exhibit a vertical branching structure to minimize nesting and perching opportunities (Figure 1).



Table 1. Design Guidance for Plant Materials

TREES	<p><b>Avoid/Prevent Contiguous Canopy</b></p> <ol style="list-style-type: none"> <li>1. Prevent overlapping crown structures. Contiguous crowns can provide safe passage for wildlife. Provide sufficient distance between plants to ensure that at least 15 feet of open space will remain between mature crowns (Figure 1).</li> <li>2. Prevent homogenous canopy types and tree height. Variable canopy height will reduce thermal cover and protection from predators.             <ul style="list-style-type: none"> <li>■ Provide significant variation between the type of canopy and height of the species, both at planting and at maturity.</li> <li>■ Provide no more than 20% evergreen species on site, and never plant evergreens in mass or adjacent to each other.</li> </ul> </li> </ol>
SHRUBS/ACCENTS/GRASSES	<p><b>Limit Coverage</b></p> <p>Limit the amount of cover and avoid massing to prevent the creation of habitat for birds or small mammals.</p> <ul style="list-style-type: none"> <li>■ Mix deciduous, herbaceous, and evergreen species.</li> <li>■ Do not plant species in mass. At a minimum, provide sufficient spacing to equal the width of each species at maturity. Avoid species with the potential to creep near shrubs (Figure 2).</li> <li>■ Provide at least 10 feet between trees and other species greater than 1 foot in height.</li> </ul>
GROUND COVER/TURF	<p><b>Prevent the natural succession of landscape!</b></p> <p>Groundcover plays a transitional role between shrubs, grasses, and trees, and this succession creates an ideal habitat for diverse wildlife (see Figure 2).</p> <ol style="list-style-type: none"> <li>1. Provide a buffer and sharp edges between groundcover, turf, shrubs and trees, using hardscape or mulching.</li> <li>2. When possible, use alternative groundcovers, such as decorative paving and hardscapes instead of planted groundcover/turf.</li> <li>3. The use of groundcover/turf may be impractical or undesirable based on irrigation needs or site-specific conditions. Consider using the following:             <ul style="list-style-type: none"> <li>■ Artificial turf in place of groundcover, which can reduce maintenance and eliminate irrigation needs (Figure 2A).</li> <li>■ Porous concrete to cover smaller areas (Figure 2B).</li> <li>■ Permeable pavers to provide visual interest while promoting drainage (Figure 2C).</li> </ul> </li> </ol>
VINES	<p><b>Limit Coverage</b></p> <p>Limit the amount of cover and avoid massing to prevent the creation of habitat for birds or small mammals.</p> <ul style="list-style-type: none"> <li>■ Do not use vines to create overhead canopy or to cover structures.</li> <li>■ Do not plant vines to grow on the trunk or branches of trees.</li> <li>■ Minimize vines to areas of 5 feet or less in width. Vines require considerably more maintenance than other plant materials.</li> </ul>

Acceptable plants from the Riverside County Landscaping Guide



## LANDSCAPING NEAR AIRPORTS: Special Considerations for Preventing or Reducing Wildlife Hazards to Aircraft

Landscaping makes a visual statement that helps to define a sense of space by complementing architectural designs and contributing to an attractive, inviting facility. In some cases, a landscaping plan can be used to restore previously disturbed areas. However, such landscape plans are not always appropriate near airports.

Wildlife can pose hazards to aircraft operations, and more than 150 wildlife strikes have been recorded at Riverside County. The Riverside County Airport Land Use Commission (ALUC) prepared this guidance for the preparation of landscape designs to support FAA's efforts to reduce wildlife hazards to aircraft. This guidance should be considered for projects within the Airport Influence Area (AIA) for Riverside County Airports. The following landscape guidance was developed by planners, landscape architects and biologists to help design professionals, airport staff, and other County departments and agencies promote sustainable landscaping while minimizing wildlife hazards at Riverside County's public-use airports.

**Discouraging Hazardous Wildlife.** Plant selections, density, and the configuration of proposed landscaping can influence wildlife use and behavior. Landscaping that provides a food source, perching habitat, nesting opportunities, or shelter can attract raptors, flocking birds, mammals and their prey, resulting in subsequent risks to aviators and the traveling public.



Figure 2. Alternative hardscapes and groundcover/turf.





**Acceptable**  
The trees above have a vertical branching structure that minimizes perching and nesting opportunities.



**Not acceptable**  
Examples of trees that are attractive to birds due to their use of horizontal branching structure.



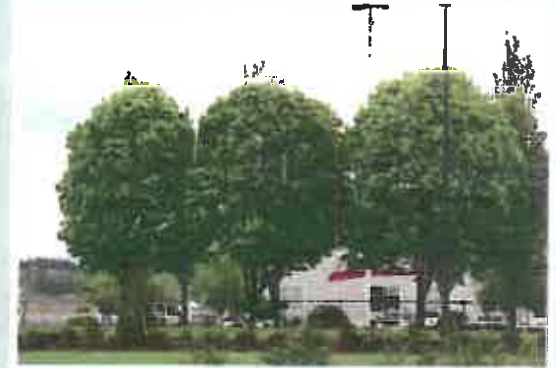
**Not acceptable**  
Trees, shrubs and plants that produce wildlife edible fruit and seeds should be avoided.



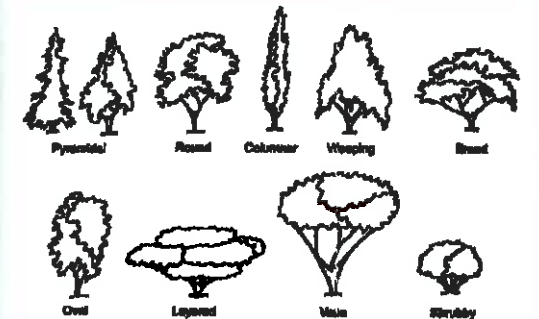
Landscaping plants to be avoided include any plants that are small, climbable with any accessibility to the canopy.

**TABLE 2 Acceptable Plants from Riverside County Landscaping Guide**

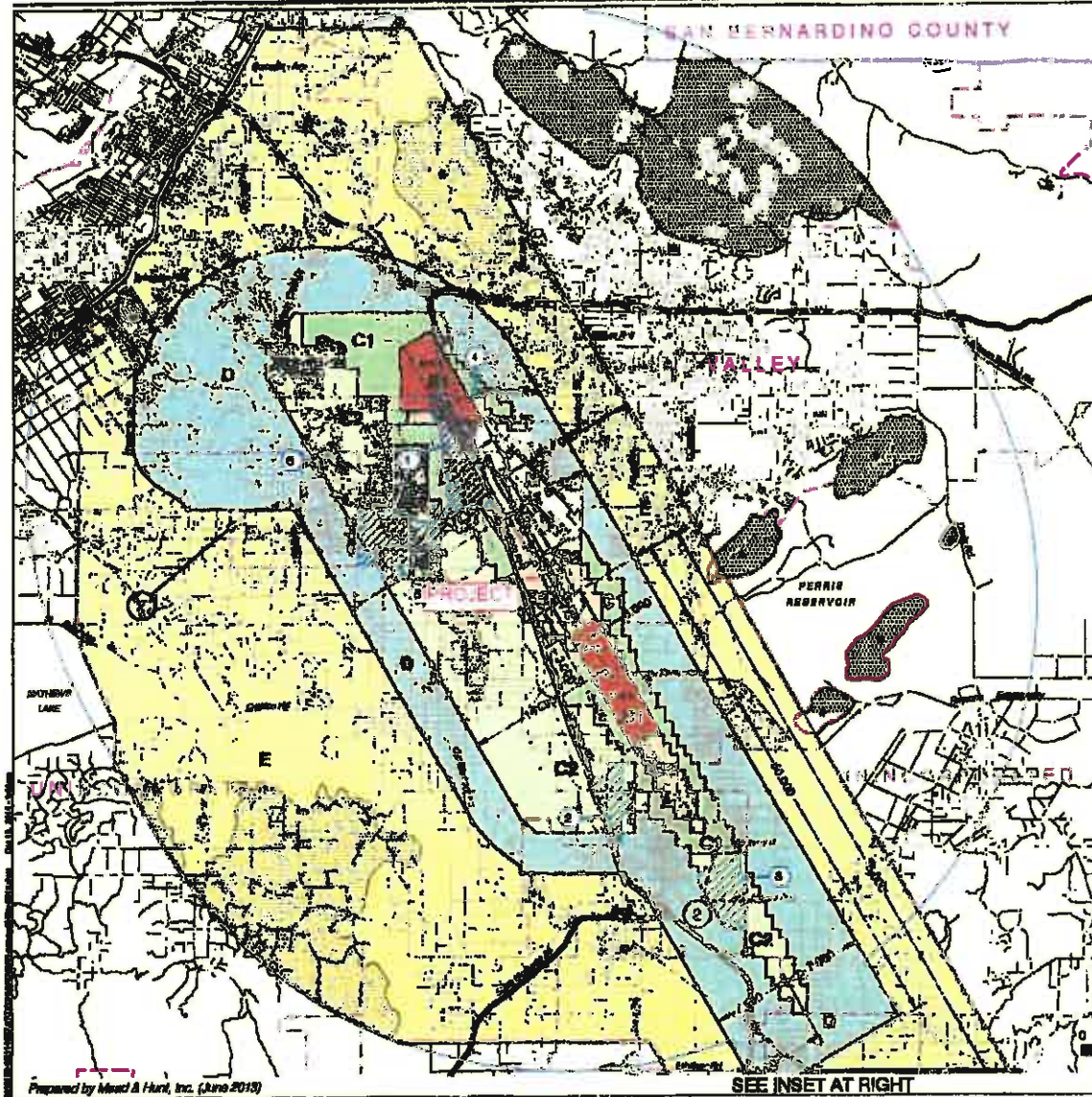
Common Name	Common Name	Wildlife Feeding Potential	Plant Height
<i>Corchorus occidentalis</i>	Western Redbud	VL: 1, 2, L: 3, 4	2-24
<i>Olea europaea</i> "Swan Hill"	Fruitless Olive	BL: 1, 2; L: 3, 4, M: 5, 6	8, 9; 11-24
<i>Pinus</i> spp.	Pine, various species	Varies by species	Varies by species
<i>Rhus typhina</i>	African Sumac	L: 1-6; M: 5-6	8-9; 12-24
<i>Robinia neocanadensis</i> *	Desert Locust	L: 1-6; M: 5-6	2-3, 7-11, 14, 18-24
<i>Robinia x auribegia</i>	Locust	L: 1-6; M: 5-6	2-24
<i>Ulmus parvifolia</i>	Chinese Elm	M: 1-6	3-24
<i>Nyctia triphylla</i>	Lemon Verbena	L: 1-6	9-10; 12-21
<i>Cistus</i> spp.	Rockrose	L: 1-6	8-9, 14-24
<i>Dalea pulchra</i>	Bush Dalea	L: 6	12, 13
<i>Encelia farinosa</i>	Brittlebush	VL: 3; L: 3-6	
<i>Gravellia Noeii</i>	Noe's Grevelia	L: 1-6; M: 6	
<i>Lespedeza cuneata</i>	Chuparosa	M: 1, 6; VL: 3; L: 4-5	
<i>Lycium carsonii</i>	Bush Santana	L: 1-4; M: 6	
<i>Lavandula</i> spp.	Lavender	L: 105; M: 5-6	2-24; varies
<i>Nandina domestica</i> species	Heavenly Bamboo	L: 1-6; M: 5-6	
<i>Rosmarinus officinalis</i> "Tuscan Blue"	Tuscan Blue Rosemary	L: 1-6; M: 5-6	
<i>Salvia greggia</i>	Autumn sage	L: 1-6; M: 5-6	
<i>Artemisia tridentata</i>	Sandhill Sage	VL: 1	
<i>Oenothera caespitosa</i>	White Evening Primrose	L: 1-2, 3-5	183, 7-14, 18-21
<i>Oenothera stubbei</i>	Baja Evening Primrose	L: 1-6	10-13
<i>Penstemon brevidentatus</i>	Del Rio	L: 4-6	10-13
<i>Trachelospermum jasminoides</i>	Star Jasmine	M: 1-6	8-24
<i>Zauschneria californica</i>	California Fuchsia	L: 1, 2, 4; VL: 3; M: 5-6	2811, 14-24
<i>Cortaderia siliqua</i> (syn. <i>C. seloana</i> )	Pampass Grass	N/A	N/A
<i>Festuca</i> spp.	Fescue	Varies by Species	Varies by Species
<i>Zoysia 'Vicaria'</i>	Zoysia Grass	80% of ETO	8-9, 12-24
<i>Agave</i> species	Agave	L: 1-4, 6	10, 12-24 (Wales)
<i>Aloe</i> species	Aloe	L: 1-4, 6	8-9, 12-24
<i>Chondropetalum hexatum</i>	Cape Rush	H: 1; M: 3	8-9, 12-24
<i>Dasylistron</i> species	Desert Spoon	VL: 1, 4-6	10-24
<i>Deschampsia cespitosa</i>	Tufted Hair Grass	L: 1-4	2-24
<i>Festuca (ovina) glauca</i>	Blue Fescue	L: 1-2; M: 3-6	1-24
<i>Nietes bicolor</i>	Fortnight Lily		VL: 1, L: 3-6
<i>Echinocactus grusonii</i>	Golden Barrel Cactus	VL: 1-2, L: 3-4, 6	12-24
<i>Fouquieria splendens</i>	Occilto	L: 1, 4-6; VL: 3	10-13, 18-20
<i>Hesperaloe parviflora</i>	Red / Yellow Yucca	VL: 3, L: 4-6	23, 3, 7-16, 18-24
<i>Muhlenbergia rigens</i>	Deer Grass	L: 1, 3; M: 2, 4-6	4-24
<i>Opuntia</i> species	Prickly Pear, Cholla	VL: 1-3; L: 4-6	Varies by Species
<i>Penstemon parryi</i>	Parry's Beardtongue	L: 1-6	10-13
<i>Penstemon superbus</i>	Superb Beardtongue	L: 1-6	10-13
<i>Tillandsia violacea</i>	Society garlic	M: 1-4, 6	13-24
<i>Yucca</i> species	Yucca	L: 1-6	Varies by Species



Not recommended are trees that overlap, allowing birds to move safely from tree to tree without exposure to the weather or predators.



Trees approved for planting should have varied canopy types and varied heights, both at time of planting and at maturity. A combination of the styles illustrated above is recommended.



**LEGEND**

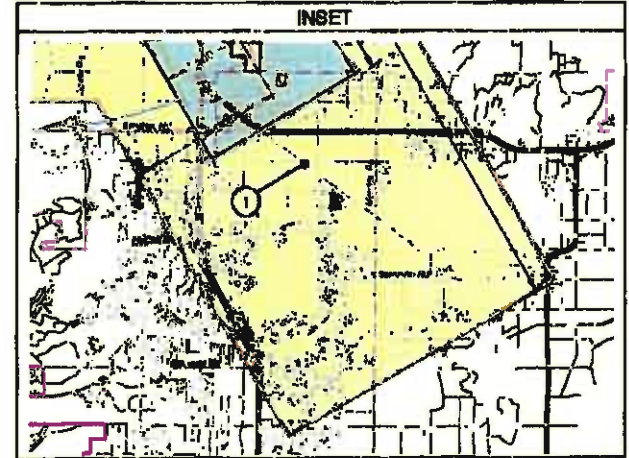
**Compatibility Zones**

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

**Boundary Lines**

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)
  - March JPA: March Business Center/Meridian
  - Perris: Harvest Landing
  - Perris: Park West
  - Moreno Valley: Affordable Housing
  - March JPA: Ben Clark Training Center
  - Riverside: Ridge Crest Subdivision

- ① Point at which aircraft on Runway 30 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,626 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.



**Riverside County  
Airport Land Use Commission  
March Air Reserve Base / Inland Port Airport  
Land Use Compatibility Plan  
(Adopted November 13, 2014)**

**Notes:**  
All dimensions are measured from runway ends and centerlines.



Base map source: County of Riverside 2013

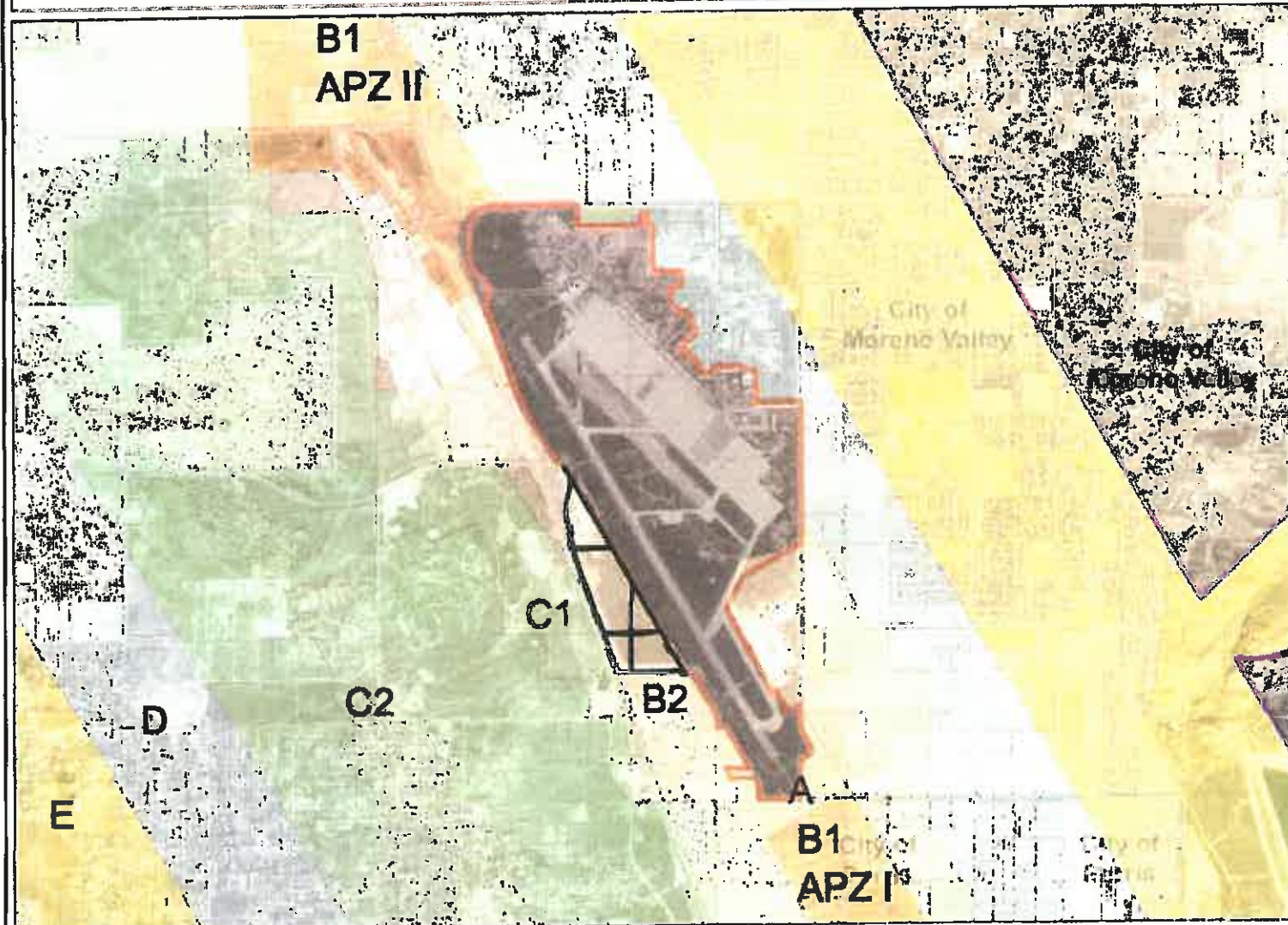
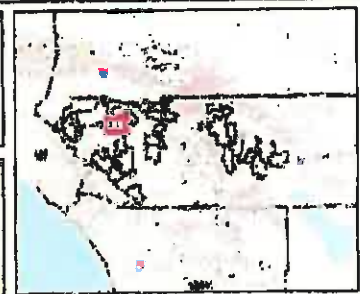
Map MA-1  
**Compatibility Map**  
March Air Reserve Base / Inland Port Airport

Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT



# My Map



**Legend**

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC6
- C2-EXC8
- C2-HIGHT



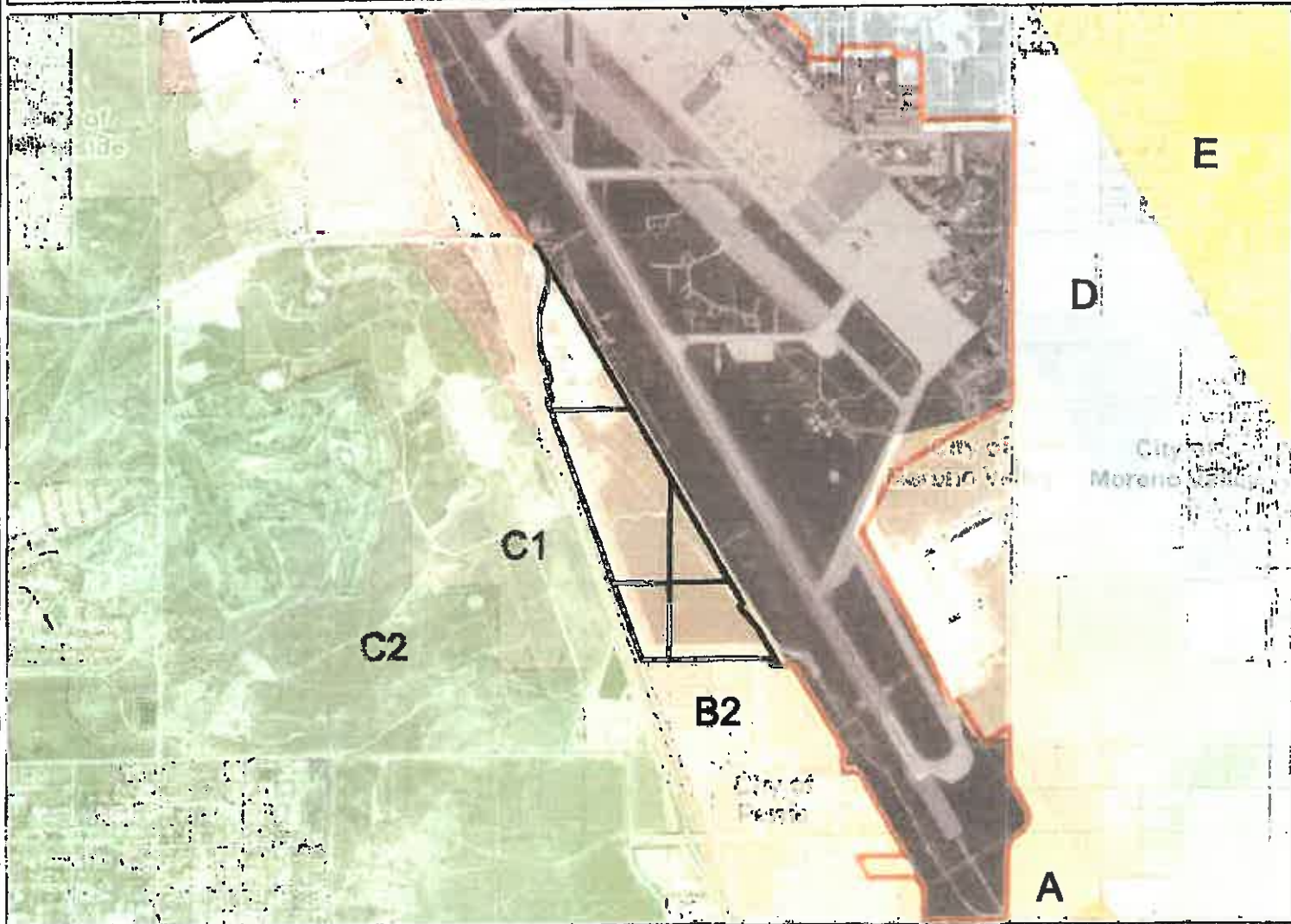
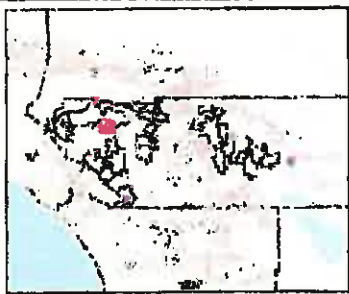
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**Notes**

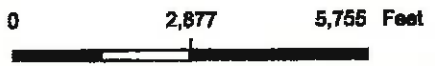
# My Map



### Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
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- C2-EXC8
- C2-HIGHT

### Notes

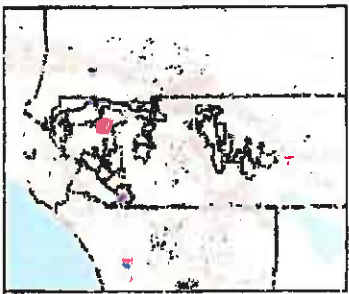


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# My Map



### Legend

- City Boundaries
- Cities
- roads
- highways

  - HWY
  - INTERCHANGE
  - INTERSTATE
  - OFFRAMP
  - ONRAMP
  - USHWY

- roads

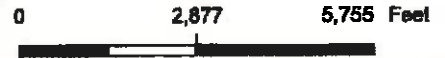
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  - Arterial
  - Collector
  - Residential

- counties
- cities
- hydrography
- waterbodies

  - Lakes
  - Rivers



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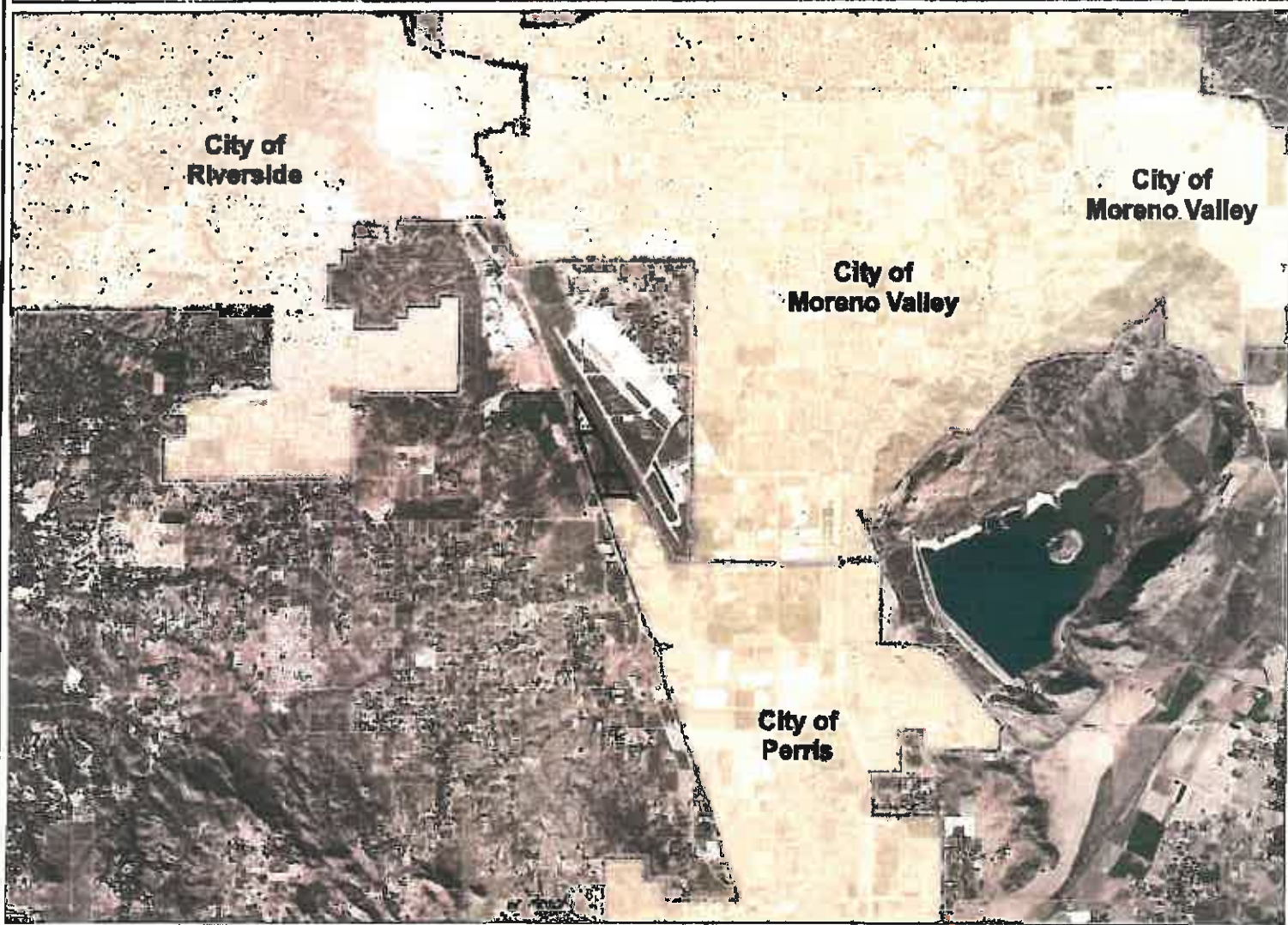
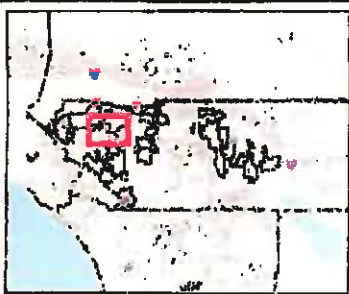


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**Notes**

# My Map



**Legend**

- City Boundaries
- Cities**
- adjacent\_highways**
  - Interstate
  - Interstate 3
  - State Highways: 60
  - State Highways 3
  - US HWY
  - OUT
- highways\_large**
  - HWY
  - INTERCHANGE
  - INTERSTATE
  - US:HWY
- counties
- cities



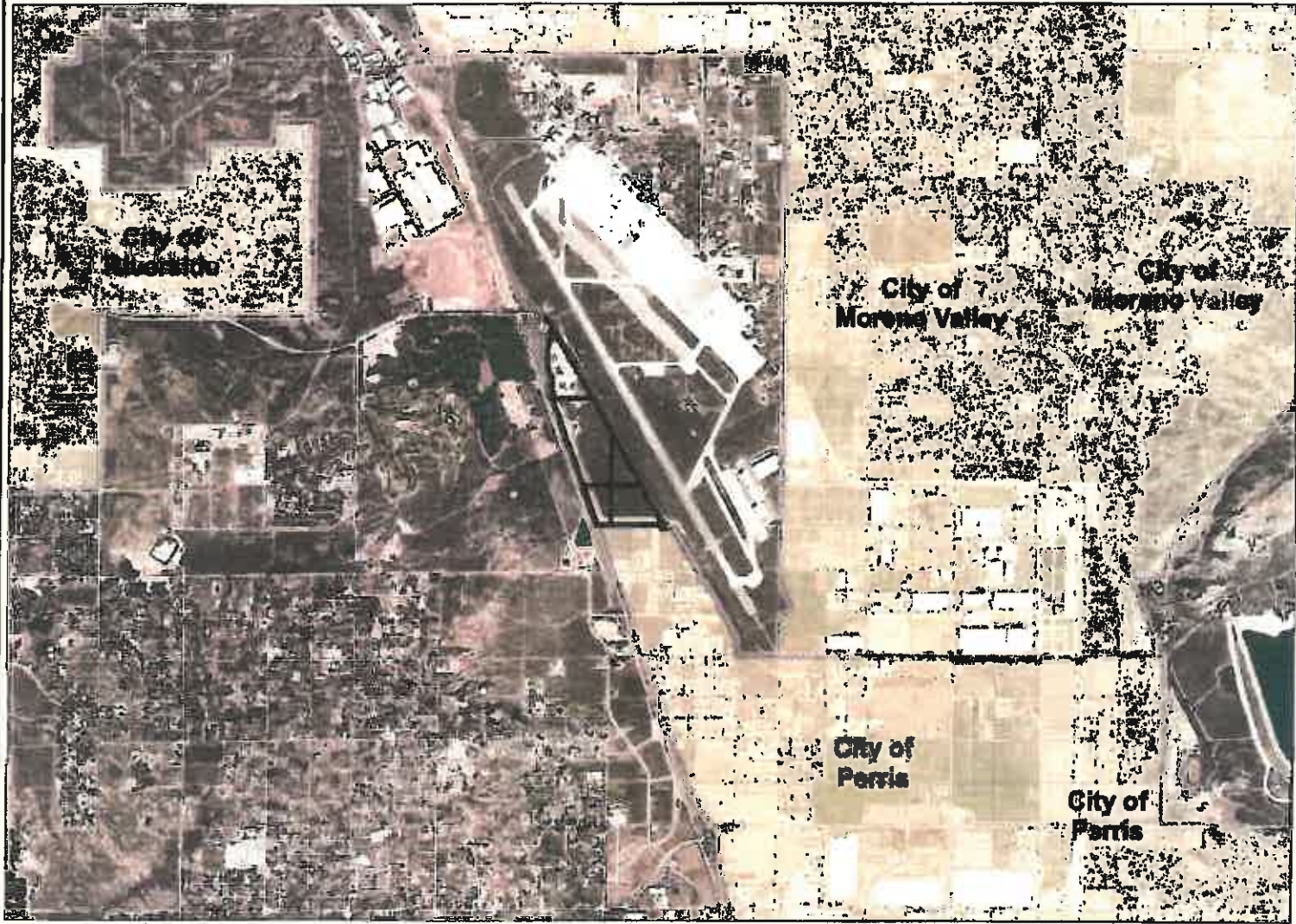
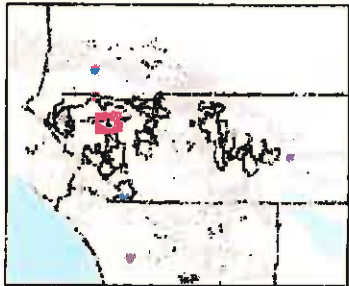
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**Notes**

# My Map



### Legend

- City Boundaries
- Cities
- highways\_large
  - HWY
  - INTERCHANGE
  - INTERSTATE
  - USHWY
- majorroads
- counties
- cities
- hydrographylines
- waterbodies
  - Lakes
  - Rivers



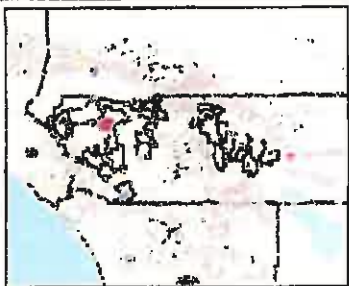
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### Notes

# My Map



## Legend

- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities
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- waterbodies
- Lake
- River

## Notes



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**NOTICE OF PUBLIC HEARING**  
**RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**  
[www.rcaluc.org](http://www.rcaluc.org)

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact ALUC Planner Paul Rull at (951) 955-6893. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The March Joint Powers Authority may hold hearings on this item and should be contacted on non-ALUC issues. For more information please contact March Joint Powers Authority Planner Mr. Jeff Smith at (951) 656-7000.

The proposed project application may be viewed at [www.rcaluc.org](http://www.rcaluc.org). Written comments may be submitted to the Riverside County ALUC by e-mail to [prull@rvco.org](mailto:prull@rvco.org) or by U.S. mail to Riverside County Administrative Center, 4080 Lemon Street, 14<sup>th</sup> Floor, Riverside, California 92501. Individuals with disabilities requiring reasonable modifications or accommodations, please telephone Barbara Santos at (951) 955-5132.

**PLACE OF HEARING:** Riverside County Administration Center  
4080 Lemon Street, 1<sup>st</sup> Floor Board Chambers  
Riverside California

**DATE OF HEARING:** May 14, 2020

**TIME OF HEARING:** 8:30 A.M.

Pursuant to Executive Order N-29-20, this meeting will be conducted by teleconference. Public access to the meeting location will be allowed, but limited to comply with the Executive Order. Residents are encouraged to view the Airport Land Use Commission meeting via Livestream on our website at [www.rcaluc.org](http://www.rcaluc.org) or on channels Frontier Fics channel 36 and AT&T U-Verse channel 59. The public may join and speak by telephone conference. Toll free number at (669) 900-6833, Zoom Meeting ID. 948 2720 1722. Passcode 011630. Zoom participants are requested to log-in 30 minutes before the meeting. Further information on how to participate in the hearing will be available on the ALUC website listed above.

**CASE DESCRIPTION:**

ZAP1405MA20 – Riverside Inland Development, LLC/Hillwood Investment Properties (Representative: Kathy Hoffman) – March Joint Powers Authority Case Nos. PP20-02 (Plot Plan), TPM20-02 (Tentative Parcel Map No. 37220). The applicant proposes to construct a 2,022,364 square foot industrial warehouse building (maximum 54 feet in height) with mezzanines on 142.5 acres located easterly of Interstate 215, southerly of March Field Air Museum and the easterly terminus of Van Buren Boulevard, northerly of Nandina Avenue, and westerly of the runways at March Air Reserve Base. The applicant also proposes to change the Veterans Industrial Park 215 Specific Plan (SP16-02), updating Section 4.3 Landscaping Guidelines to reflect ALUC wildlife hazard goals and policies. The applicant also proposes to merge the project's five parcels into one parcel. (A previous proposal to establish two industrial buildings (maximum 48 feet in height) totaling 2,185,618 square feet on this site had been found consistent by the ALUC, but no action was taken by the March Joint Powers Authority Commission) (Airport Compatibility Zone B2 of the March Air Reserve Base/Inland Port Airport Influence Area).

ALUC wildlife hazard goals and policies. The applicant also proposes to merge the project's five parcels into one parcel. (A previous proposal to establish two industrial buildings (maximum 48 feet in height) totaling 2,185,618 square feet on this site had been found consistent by the ALUC, but no action was taken by the March Joint Powers Authority Commission) (Airport Compatibility Zone B2 of the March Air Reserve Base/Inland Port Airport Influence Area).





# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

## APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ~~ZAP1405MAZ0~~ **ZAP1405MAZ0** DATE SUBMITTED: February 21, 2020

### APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

Applicant	Riverside Inland Development, LLC	Phone Number	909-382-0033
Mailing Address	901 Via Piemonte Suite 175 Ontario, CA 91764	Email	
Representative	Kathy Hoffer - Hillwood	Phone Number	909-382-0033
Mailing Address	901 Via Piemonte Suite 175 Ontario, CA 91764	Email	Kathy.hoffer@hillwood.com
Property Owner	March Joint Powers Authority	Phone Number	951-656-7000
Mailing Address	14205 Meridian Parkway Suite 146 Riverside, CA 92518	Email	

March  
B2

### LOCAL JURISDICTION AGENCY

Local Agency Name	March Joint Powers Authority	Phone Number	951-656-7000
Staff Contact	Jeff Smith	Email	smith@marcjpa.com
Mailing Address	14205 Meridian Parkway Suite 146 Riverside, CA 92518	Case Type	Plot Plan Review
Local Agency Project No		<input type="checkbox"/>	General Plan / Specific Plan Amendment
		<input type="checkbox"/>	Zoning Ordinance Amendment
		<input type="checkbox"/>	Subdivision Parcel Map / Tentative Tract
		<input type="checkbox"/>	Use Permit
		<input checked="" type="checkbox"/>	Site Plan Review/Plot Plan
		<input type="checkbox"/>	Other

### PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address	Van Buren and I-215 at March Inland Port Airport		
Assessor's Parcel No.	284-160-008, 284-170-005, 285-300-008, 284-140-013, 284-180-038	Gross Parcel Size	142.5 acres
Subdivision Name		Nearest Airport and distance from Airport	Un-Zoned
Lot Number			

### PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe) The existing General Plan land use designation is aviation. The site is located within zone B2.

Proposed Land Use (describe)	The existing General Plan land use designation of aviation would be expanded to include general warehousing and logistics uses.		
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)	NA	
For Other Land Uses (See Appendix C)	Hours of Operation	24/7-Operation	
	Number of People on Site	Maximum Number	100 people per acre, average on-site
	Method of Calculation	Per Specific Plan	
Height Data	Site Elevation (above mean sea level)	Please see attached Exhibit A ft.	
	Height of buildings or structures (from the ground)	55 ft.	
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
	If yes, describe	_____	

- A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:**
1. . . . . Completed ALUC Application Form
  1. . . . . ALUC fee payment
  1. . . . . Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
  1. . . . . Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
  1. . . . . CD with digital files of the plans (pdf)
  1. . . . . Vicinity Map (8.5x11)
  1. . . . . Detailed project description
  1. . . . . Local jurisdiction project transmittal
  3. . . . . Gummed address labels for applicant/representative/property owner/local jurisdiction planner
  3. . . . . Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. (Only required if the project is scheduled for a public hearing Commission meeting)

**COUNTY OF RIVERSIDE  
AIRPORT LAND USE COMMISSION**

**STAFF REPORT**

**AGENDA ITEM:** 3.1 ~~3-6-2-2~~

**HEARING DATE:** *August 13, 2020 (continued from February 13, 2020, January 9, 2020)*

**CASE NUMBER:** ZAP1393MA19 – Innovation Industrial Partners/Vincent Von Der Ahe (Representative: Kent Norton, MIG. Inc.)

**APPROVING JURISDICTION:** March Joint Powers Authority

**JURISDICTION CASE NO:** PP19-03 (Plot Plan)

**LAND USE PLAN:** 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan (March ALUCP)

**Airport Influence Area:** March Air Reserve Base

**Land Use Policy:** Compatibility Zones B1-APZ-I and B2

**Noise Levels:** 65-70 CNEL

**MAJOR ISSUES:** The applicant is proposing to construct an industrial warehouse building on a site located partially within the portion of Airport Compatibility Zone B1 in Accident Potential Zone I (APZ-I), as delineated by the United States Air Force in the 2005 and 2018 Air Installation Compatible Use Zone (AICUZ) studies. (The remainder of the site is located in Zone B2.) The proposed building floor area would be expected to accommodate a population level that is inconsistent with ALUC's Compatibility Zones B1-APZ-I and B2 average and single acre intensity limit. ~~average intensity limit of 25 people per acre and B2 limit of 100 people per acre.~~ However, the potential occupancy would also exceed 25 people in any given acre, which the Air Force understands to be the maximum allowable level pursuant to Department of Defense Instruction (DoDI) No. 4165.57.

In order to address this issue, the applicant ~~has accepted~~ *agreed to accept* a condition to execute and record a Covenant on the title of the property, restricting actual occupancy of the building to a maximum of 25 people in any given acre in APZ-I. Operation in compliance with this covenant will be necessary to satisfy Air Force and March Joint Powers Authority concerns regarding project intensity. ~~With the Covenant in place, the project's intensity becomes consistent with Compatibility Zone B1-APZ-I average and single acre intensity criteria, and with the Air Force DoDI intensity criteria.~~

*The Air Force completed its review of the project and provided comments on July 1, 2020, stating concurrence with the project as long as the requirement for the Covenant restricting occupancy to the building to a maximum 25 people in any given acre is still in place. The requirement of the Covenant is still part of the project's recommended conditions, and the applicant has re-confirmed their acceptance of such condition.*

~~*At the January 9, 2020 meeting, the Commission voted against a motion for consistency relying on the Covenant as a means of complying with the March ALUCP average intensity limits. The Commission voted to continue the item to the February 13 meeting, pending Air Force comments. The project team has since requested continuance to the March meeting.*~~

~~*At the February 13, 2020 meeting, the Commission voted to continue the project off-calendar to allow the applicant additional time to revise the project so that it complies with the March ALUCP criteria. The applicant has reduced the portion of the building located in the APZ-I so that the project complies with the March ALUCP average intensity limits. against a motion for consistency relying on the Covenant as a means of complying with the March ALUCP average intensity limits. The Commission voted to continue the item to the February 13 meeting, pending Air Force comments. The project team has since requested continuance to the March meeting.*~~

**RECOMMENDATION:** Staff recommends that the Commission find the proposed Plot Plan CONSISTENT, subject to the conditions included herein. ~~and such additional conditions as may be required by the Federal Aviation Administration Obstruction Evaluation Service. Staff recommends that the Commission CONTINUE the matter to the March 12, 2020 meeting, pending completion of the Air Force review of the project and per the applicant's request for additional time to resolve intensity issues.~~

**PROJECT DESCRIPTION:** The applicant proposes to construct a ~~48,400~~ **48,830** square foot industrial warehouse building on 3.22 acres.

*The original project design presented at the January 9 and February 13, 2020, hearings has been significantly redesigned, with a reduction in the building size area located in the B1-APZ-I portion of the site, and the placement of office areas in the B2 portion of the site.*

**PROJECT LOCATION:** The site is located on the southeast corner of Cactus Avenue and Innovation Drive, approximately 4,670 feet northwesterly of the northwesterly terminus of Runway 14-32 at March Air Reserve Base.

#### **BACKGROUND:**

Site-Specific Exception Area: The project is located within the March Joint Powers Authority: March Business Center Specific Plan and Meridian site exception area as identified in the 2014 March ALUCP. This exception area consisted of properties that were subject to entitlements (SP-1 and SP-5) with development agreements in effect prior to the adoption of the 2014 March ALUCP. The March ALUCP, therefore, included language that exempted subsequent projects in these areas

from compliance with March ALUCP compatibility criteria and ALUC review ~~The exception was only to be valid~~ as long as the indicated specific plans and associated development agreements remained in effect.

Pursuant to the provisions stated in the March Joint Powers Authority letter dated November 8, 2017 (included in this staff report package), the development agreement no longer applies to this property. The project, therefore, is subject to ALUC review and the 2014 March ALUCP compatibility criteria.

Non-Residential Land Use Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zones B1-APZ-I and B2. Zone B1-APZ-I limits average intensity to 25 people per acre, and B2 limits average intensity to 100 people per acre. Approximately 2.55 acres are located within APZ-I and 0.67 acres within B2.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, the following rates were used to calculate the occupancy for the proposed project:

- Office – 1 person/200 square feet, and
- Warehouse – 1 person/500 square feet.

The applicant proposes a ~~48,400~~ **48,830** square foot industrial warehouse building consisting of ~~43,400~~ **39,230** square feet of warehouse area and ~~5,000~~ **4,800** square feet of **first floor** office area, **and 4,800 square feet of second floor office mezzanine area**, potentially accommodating ~~112~~ **127** people, resulting in an average of ~~35~~ **39** people per acre for the entire site, which would be inconsistent with the Compatibility Zone B1-APZ-I criterion of 25 (but consistent with Compatibility Zone B2 criterion of 100).

A breakdown of use by Compatibility Zone indicates that ~~40,258~~ **29,988** square feet of warehouse area ~~and 5,000 square feet of office area~~ would be located within Zone B1-APZ-I, potentially accommodating ~~106~~ **60** people, resulting in an average intensity of ~~42~~ **24** people per acre for the portion of the site located in Zone B1-APZ-I, which would be inconsistent with the Compatibility Zone B1-APZ-I average acre intensity criterion of 25. **Approximately 3,142 9,242** square feet of warehouse area, **4,800 square feet of first floor office area, and 4,800 square feet of second floor office mezzanine area** would be located in Zone B2, accommodating **67** people, resulting in an average intensity of ~~9~~ **100** people per acre for the portion of the site located in Zone B2, which would be consistent with the Compatibility Zone B2 average acre intensity criterion of 100.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle in the absence of more precise data). Based on the number of parking spaces provided (~~79~~ **71** standard vehicle spaces), the total occupancy would be estimated at ~~119~~ **107** people for an average intensity of ~~37~~ **33** people per acre, which is inconsistent with the Zone B1-APZ-I average acre intensity criterion of 25, but consistent with the B2 average intensity criterion of 100.

The applicant has agreed to a condition that will require execution and recordation of a Covenant, recorded on the title of the property, which limits actual occupancy of the building in conformance with the limit of 25 persons in any given acre within APZ-I. ~~With this Covenant in place, the project's occupancy accommodates 28 people, resulting in an average intensity of 9 people per acre for the entire site, which is consistent with average intensity criteria for Zone B1-APZ-I of 25 people per acre, and Zone B2 of 100 people per acre. (However, this would also indicate that the number of parking spaces being required is excessive.)~~

Non-Residential Single-Acre Land Use Intensity: Compatibility Zone B1-APZ-I limits maximum single-acre intensity to 100 people, and Zone B2 limits single-acre intensity to 250 people. There are no risk-reduction design bonuses available, as March Air Reserve Base/Inland Port Airport is primarily utilized by large aircraft weighing more than 12,500 pounds.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre intensity ~~are as follows. occurs around the proposed office areas in APZ-I. This~~ **The portion of the single-acre area located within Compatibility Zone B1-APZ-I includes 31,647 23,688 square feet of warehouse area resulting in 47 people, and the portion of the single acre-acre located within Compatibility Zone B2 includes 9,242 square feet of warehouse area, 4,800 square feet of first floor office area, and 4,800 square feet of second floor office mezzanine area resulting in 67 people, and 5,000 square feet of office area, which, in the absence of the Covenant, could accommodate a total occupancy of 88 people, both of which would be are** consistent with the 2014 March ALUCP Compatibility Zones B1-APZ-I and B2 single acre intensity criterion of 100 and 250. (as well as the Zone B2 criterion of 250) (Approximately 6,913 5,830 square feet of the single-acre area is located outside the building and will not generate any occupancy.)

Although the abovementioned single acre intensities are consistent with the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, they are inconsistent with the Air Force's Department of Defense Instruction No. 4165.57 with regard to intensity, which is limited to a maximum of 25 people in any given acre in APZ-I. A more detailed analysis is provided below in the March Air Reserve Base section of the staff report.

March Air Reserve Base/United States Air Force Input: Given that the project site is located in Zones B1-APZ-I and B2 of the primary runway at March Air Reserve Base, the March Air Reserve Base staff was notified of the project and sent a package of plans for their review. ~~As of the time this staff report was prepared, we were still awaiting comments from the Air Force regarding this project.~~ ***Comments from the March Air Reserve Base staff was provided on July 1, 2020, and are included in this staff report package.***

The 2018 Airport Installation Compatible Use Zones (AICUZ) study identifies most of the project site as located within Accident Potential Zone I (APZ-I). Appendix A of the AICUZ provides Land Use Compatibility Tables for the APZs, which cite "warehousing" as a permitted use in APZ-I (and prohibited use in the Clear Zone [CZ]).

However, March Air Reserve Base officials maintain that the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan is not consistent with current Air Force guidance found in Air Force Instruction 32-7063 dated December 18, 2015, which addresses Air Force policies on Land Use Compatibility in accordance with Department of Defense Instruction (DoDI) No. 4165.57. These inconsistencies include conflicts with regard to lot coverage, intensity, and permitted use definitions.

The proposed project complies with the restrictions on permitted uses and lot coverage, but not with the intensity limits. The Air Force understands the DoDI criteria as limiting intensity to a maximum of 25 people in any given acre of APZ-I. As noted above, the project would be expected to result in a single acre occupancy of ~~88~~ 47 people **within the APZ-I portion**.

The projected occupancy intensities would be inconsistent with the Air Force intensity understanding.

One method of bringing the project into consistency with ~~both the March ALUCP and~~ the Air Force Instruction (AFI) is for the applicant to agree to a condition including a Covenant, recorded on the title of the property, restricting the actual occupancy of the building to the limits of the AFI.

The applicant has agreed to a condition that will require execution and recordation of said document, which limits actual occupancy of the building in conformance with the limit of 25 persons in any given acre within APZ-I. Specifically, the Covenant states:

E. Covenanter has agreed to comply with the Density Restriction and a Density Cap (both terms are defined below), by limiting occupancy of the Project to (i) ~~forty-eight (48)~~ **thirty-eight (38)** occupants (“Density Cap”); and (ii) twenty-five (25) occupants in any square area measuring 208 feet by 208 feet (“Square Area”) for all Square Areas within portions of the building of the Project within APZ I. Requirement (ii) constitutes the “Density Restriction”. Accordingly, any building expansion or change in use is prohibited, without further review by the JPA and MARB representatives, and consent and approval provided through an amendment to this covenant.

The Density Cap of ~~48~~ **38** in this situation was derived by subtracting the portion of the building in the most intense single-acre area from the total square footage, applying the one person per 500 square feet ratio to that area, and adding that number (~~23~~ **13**) to the 25 in the most intense single-acre area.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zone B1-APZ-I and B2. Industrial warehouse buildings are compatible within Accident Potential Zone I pursuant to the 2018 Air Installation Compatible Use Zone (AICUZ) study disseminated by the United States Air Force. Use as an industrial warehouse is also compatible pursuant to Department of Defense Instruction (DODI) No. 4165.57, but the intensity levels of this project in the absence of the Covenant would exceed DODI allowances, as understood by the Air Force.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being in an area between 65-70 CNEL range from aircraft noise. As a primarily industrial use not sensitive to noise (and considering typical anticipated building construction noise attenuation of approximately 20 dBA), the warehouse area would not require special measures to mitigate aircraft-generated noise. However, a condition is included to provide for adequate noise attenuation within office areas of the building so as to achieve an interior noise level of 45 CNEL.

Part 77: The elevation of Runway 14-32 at its northerly terminus is 1,535 feet above mean sea level (1,535 feet AMSL). At a distance of approximately 4,670 feet from the runway to the closest parcel within the site, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,581 feet AMSL. The maximum finished floor elevation is 1,562 feet AMSL. The applicant has identified that all building heights will be a maximum of 38 feet, resulting in a top point elevation of 1,600 feet AMSL. Therefore, review of this building by the FAA Obstruction Evaluation Service (FAA OES) is required. Submittal to the FAA OES was made, and Aeronautical Study Number 2019-AWP-15121-OE was assigned to this project. ~~Its status is currently a “work in progress”.~~ **A Determination of No Hazard to Air Navigation letter was issued on January 3, 2020. The FAA OES determined that the project would not result in an impact to air navigation, provided that the project complies with the conditions in that letter (which have been included in staff’s recommended conditions).**

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically. However, development within Compatibility Zone B1-APZ-I is limited to a maximum lot coverage of 50% (no requirement for Zone B2). The proposed lot coverage is 43 ~~27~~%, which is consistent with the maximum lot coverage criterion for warehouses of 50% in the Accident Potential Zones.

#### **CONDITIONS:**

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.



- (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, congregate care facilities, hotels/motels, places of assembly, restaurants, hazardous materials manufacture/storage, noise sensitive outdoor nonresidential uses, and hazards to flight.
  - (f) Retail trade, eating and drinking establishments, personal services, professional services, educational services, governmental services, medical facilities, cultural activities, and any other uses providing on-site services to the public.
  - (g) Commercial service uses; civic uses; churches, chapels, and other places of worship; classrooms; gymnasiums; theaters; conference or convention halls; auditoriums; fraternal lodges; gaming; auction rooms.
  - (h) Manufacturing of: food and kindred products, textile mill products, apparel, chemicals and allied products, rubber and plastic products, fabricated metal products, professional, scientific, and controlling instruments, photographic and optical goods, watches and clocks.
3. Prior to issuance of any building permits, the landowner shall convey and have recorded an avigation easement to the March Inland Port Airport Authority. Contact March Joint Powers Authority at (951) 656-7000 for additional information.
  4. The attached notice shall be given to all prospective purchasers of the property and/or tenants of the building. While not required, the applicant and its successors-in-interest are encouraged to provide a copy of said notice to employees who would regularly be working at this location.
  5. Any proposed detention basins on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the retention basin(s) that would provide food or cover for bird species that would be

incompatible with airport operations shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the retention basin(s) shall not include trees that produce seeds, fruits, or berries.

6. This project has been evaluated as a proposal for ~~43,400~~ **39,230** square feet of warehouse area and ~~5,000~~ **9,600** square feet of office floor area. March Joint Powers Authority shall require additional review by the Airport Land Use Commission prior to the establishment of office uses exceeding the amounts specified above.
7. Office space must have sound attenuation features sufficient to reduce interior noise levels from exterior aviation-related sources to no more than CNEL 45 dB. March Joint Powers Authority shall require an acoustical study to ensure compliance with this requirement.
8. Zoned fire sprinkler systems shall be required throughout the building.
9. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
10. The project shall execute and record a Covenant on the title of the property, which limits building occupancy to a maximum of 25 people in any given acre in the APZ-I portion of the building. The project shall be in compliance with the recorded Covenant. Any changes to the Covenant will require review by the Airport Land Use Commission, March Joint Powers Authority, and March Air Reserve Base.
11. **The Federal Aviation Administration has conducted an aeronautical study of the proposed project (Aeronautical Study No. 2019-AWP-15121-OE) and has determined that neither marking nor lighting of the structure is necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 L Change 2 and shall be maintained in accordance therewith for the life of the project.**
12. **The proposed building shall not exceed a height of 38 feet above ground level and a maximum elevation at top point of 1,600 feet above mean sea level.**
13. **The maximum height and top point elevation specified above shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in structure height or elevation shall**

**not require further review by the Airport Land Use Commission.**

- 14. Temporary construction equipment used during actual construction of the structure(s) shall not exceed 38 feet in height and a maximum elevation of 1,600 feet above mean sea level, unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.**
- 15. Within five (5) days after construction of the proposed building reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://ocaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned or a decision is made not to construct the applicable structure.**

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

Rull, Paul

**From:** Pacino, Brian <Brian.Pacino@jacobs.com>  
**Sent:** Wednesday, July 1, 2020 5:12 PM  
**To:** WATERS, DOUGLAS S GS-13 USAF AFRC 452 MSG/CE  
**Cc:** Rull, Paul  
**Subject:** 9 JUL 2020 ALUC Hearing\_MARB Preliminary Comments  
**Attachments:** Meridian U1-L2\_Revised Site Plan\_062020.PDF; ZAP1390MA19\_ALUC Staff Report.doc

Doug,

In preparation for the July 9 ALUC hearing meeting and at request of Paul Rull, please note the following preliminary comments on behalf of MARB BCE. Please respond with Paul copied, with your approval and any additional input or questions.

For the Meridian U1-L2 warehouse (site almost entirely in Runway 14 APZ I) please recall our 20 May teleconference with March JPA (Jeff Smith), Developer, and ALUC, after which Developer revised the site plan per attached PDF based on your comments regarding safety concerns tied to office space intensities exceeding AF/DoD restriction levels for portions inside APZs.

I wanted to get you and Paul my preliminary review comments before heading out on leave starting tomorrow through July 14.

V/r,

Brian  
CTR, 452 MSG/CE

**FOR OFFICIAL USE ONLY:**

ALUC Case#	Development Title	Rooftop Solar?	ALUC Zone	Comments
ZAP1390MA20	City of Perris Warehouse, PR Partners LLC	No	B1 (11.8 acres in APZ II) C1 (3.9 acres)	<ul style="list-style-type: none"><li>• CONCUR with review comments and conditions to be met by developer/applicant based on ALUC staff report for ZAP1390MA20 (attached and first received by MARB on 22 June 2020) as follows:  <i>- Non-Residential Single-Acre Land Use Intensity: The proposed project complies with the restrictions on permitted uses and lot coverage, and intensity limits. The Air Force understands the DoDI criteria as limiting intensity to a maximum of 50 people in any given acre of APZ-II. As noted above, "the project would be expected to result in a single acre occupancy of 44 people in APZ-II."</i></li></ul>

				<p>- <u>AICUZ</u>: Appendix A of the 2018 MARB AICUZ study provides Land Use Compatibility Tables for the APZs, which “cite warehousing as a permitted use in APZ-II. Warehouses are also compatible pursuant to Department of Defense Instruction (DODI) No. 4165.57.”</p> <p>- <u>Noise</u>: Although the site is in the 60-70 CNEL contour range, “as a primarily industrial use not sensitive to noise (and considering typical anticipated building construction noise attenuation of approximately 20 dBA), the warehouse area would not require special measures to mitigate aircraft-generated noise. However, a condition is included to provide for adequate noise attenuation within office areas of the building.”</p> <p>- <u>FAA Part 77</u>: Concur with findings that FAA-OES investigation not required since maximum proposed building height of 36 ft. plus existing site elevation (1,464 ft.) is less than 1,571 ft. AMSL.</p> <p>- <u>Hazards to Flight/BASH</u>: Concur with ALUC’s assessment that developer/applicant proposal to install vegetative swales is suitable for BASH mitigation for a project that is located 8,300 feet from Runway 32, “the project proposes 13,247 square feet of vegetative swale area. Vegetative swales are an acceptable form of stormwater management, pursuant to the study “Wildlife Hazard Management at Riverside County Airports: Background and Policy”, and are acceptable within the vicinity of airports as they do not usually involve ponded water, provided that the proposed vegetation/landscaping are not attractive to hazardous wildlife, and that it is adequately maintained.”</p>
ZAP1393MA20	<p>Innovation Industrial Warehouse – Meridian Lot 2, Unit 1</p> <p><i>*This project is being coordinated with March JPA (POC is Jeff Smith, Sr, Planner)</i></p>	No	<p><b>B1</b> (2.4 acres in APZ I) <b>B2</b> (0.82 acres)</p>	<ul style="list-style-type: none"> <li>Conference call held between March JPA, Developer, ALUC, and MARB on 20 May 2020. Per MARB BCE concerns related to office density proposals in APZ I, the developer has revised site plan. Based on ALUC interpretation, MARB is in concurrence as follows: <ul style="list-style-type: none"> <li>- Based on this revision, the project proposes 29,988 sq.ft. of warehouse area in Zone B1, which would accommodate an occupancy of 60 people (at a warehouse ratio of 1/500), and result in an average intensity of 24</li> </ul> </li> </ul>

				<p><i>people per acre, which is consistent with Zone-B1-APZ-I average intensity criteria of 25 people per acre.</i></p> <p><i>- The project also proposes 9,242 sq.ft. of warehouse area (19 people) and 9,600 sq.ft. of office area (48 people) in Zone B2, which would accommodate an occupancy of 67 people, and result in an average intensity of 100 people per acre, which is consistent with Zone B2 average intensity criteria of 100 people per acre.</i></p> <ul style="list-style-type: none"> <li>• Despite alterations to average intensities noted above, MARB still requires that the applicant adhere to previous VDA agreement to utilize a covenant limiting occupancy of the building to 25 person in any give acre.</li> </ul>
<b>ZAP1425MA20</b>	City of Perris - Newcastle Harvill Logistics Warehouse (located easterly of Harvill Avenue, westerly of Interstate 215 Freeway, southerly of Orange Avenue, and northerly of Daytona Cove)	Yes	C2 (Not in APZs)	<ul style="list-style-type: none"> <li>• Concur with Enertis Solar Glare Hazard Analysis report (Dec. 28, 2019) and ForgeSolar PASS findings for proposed rooftop solar (yellow and green glare applicable to MARB flight tracks and ATCT), however we support analysis of cumulative impacts on airfield operations as part of upcoming Compatible Use Study in conjunction with the OEA.</li> <li>• Otherwise, concur with typical set of ALUC Development Conditions criteria to which applicant/developer compliance is required.</li> </ul>

**Brian J. Pacino, AICP | Jacobs | Buildings, Infrastructure & Advanced Facilities |**  
 949.224.7635 office | 703.627.3010 mobile | [brian.pacino@jacobs.com](mailto:brian.pacino@jacobs.com) | [www.jacobs.com](http://www.jacobs.com)

**From:** Rull, Paul <PRull@RIVCO.ORG>  
**Sent:** Monday, June 22, 2020 7:13 AM  
**To:** Pacino, Brian <Brian.Pacino@jacobs.com>  
**Cc:** WATERS, DOUGLAS S GS-13 USAF AFRC 452 MSG/CEV <douglas.waters.2@us.af.mil>  
**Subject:** [EXTERNAL] RE: ZAP1421MA20 ALUC review

Attached is a copy of the draft staff report to show you our calculations regarding intensity and review of the hazard to wildlife.

If you have any questions, please feel free to contact me.

**Paul Rull**  
 ALUC Principal Planner



Mail Processing Center  
 Federal Aviation Administration  
 Southwest Regional Office  
 Obstruction Evaluation Group  
 10101 Hillwood Parkway  
 Fort Worth, TX 76177

Aeronautical Study No.  
 2019-AWP-15121-OE  
 Prior Study No.  
 2008-AWP-4498-OE

Issued Date: 01/03/2020

Deirdre McCollister  
 MIG  
 1500 Iowa Avenue, Suite 110  
 Riverside, CA 92507

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building VDA Meridian Lot 2  
 Location: Riverside, CA  
 Latitude: 33-54-29.00N NAD 83  
 Longitude: 117-17-01.00W  
 Heights: 1562 feet site elevation (SE)  
 38 feet above ground level (AGL)  
 1600 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 07/03/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.



**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

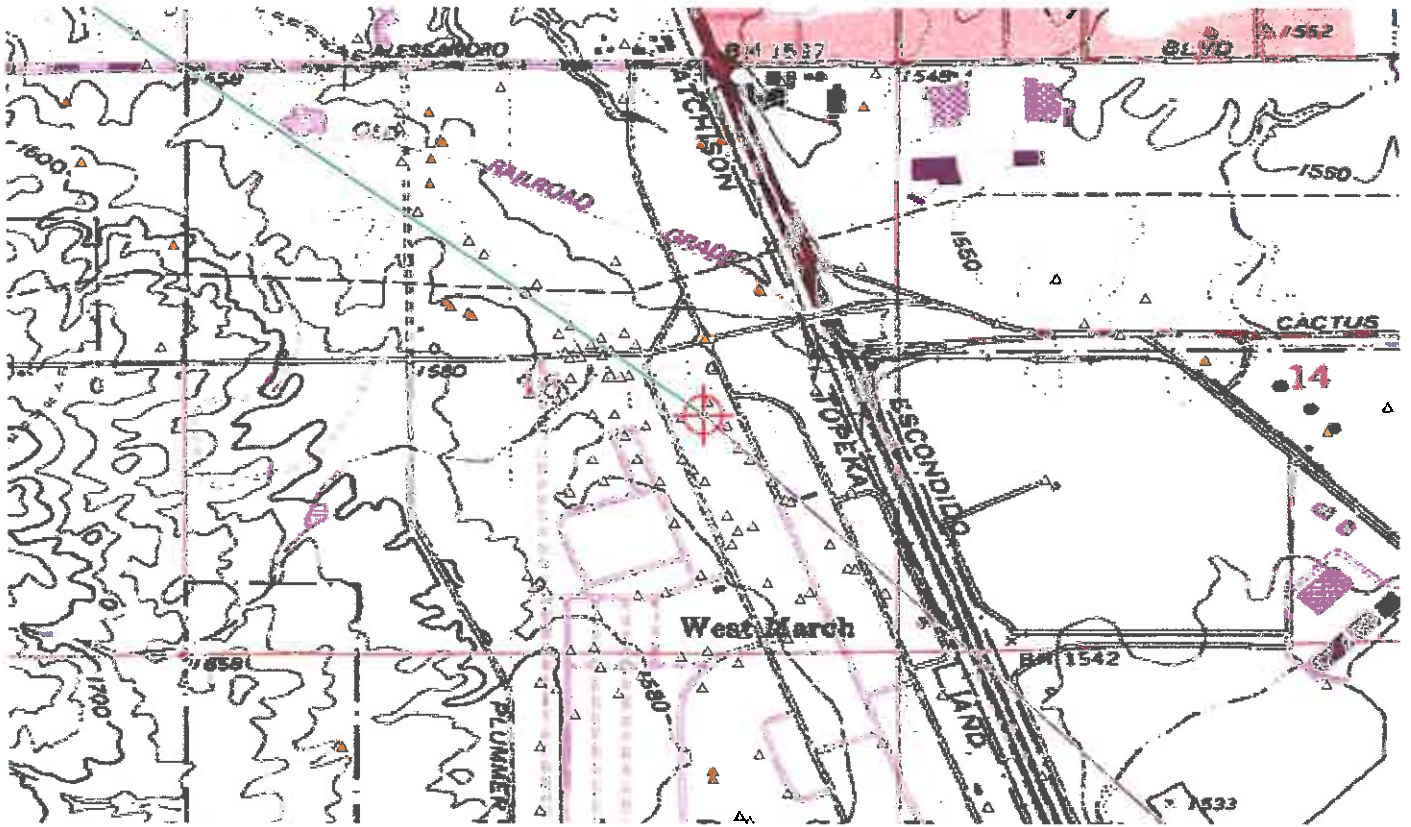
If we can be of further assistance, please contact our office at (817) 222-4613, or [natalie.schmalbeck@faa.gov](mailto:natalie.schmalbeck@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AWP-15121-OE.

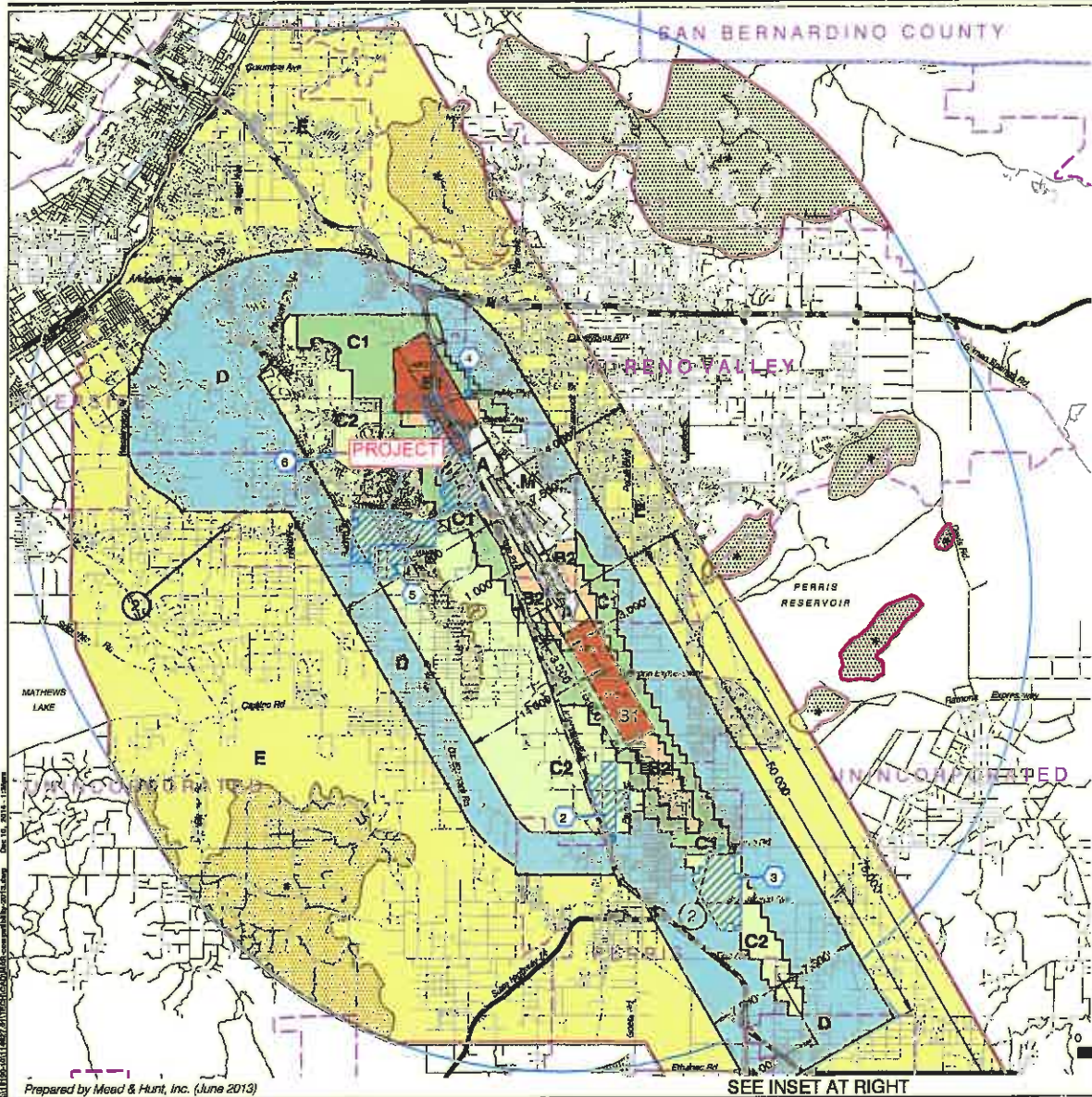
**Signature Control No: 424408877-426666492**  
Natalie Schmalbeck  
Technician

( DNE )

Attachment(s)  
Map(s)

Verified Map for ASN 2019-AWP-15121-OE





**LEGEND**

**Compatibility Zones**

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

**Boundary Lines**

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision

**INSET**



**Riverside County  
Airport Land Use Commission**

**March Air Reserve Base / Inland Port Airport  
Land Use Compatibility Plan**

(Adopted November 13, 2014)

Note:  
All dimensions are measured from  
runway ends and centerlines.



Base map source: County of Riverside 2013

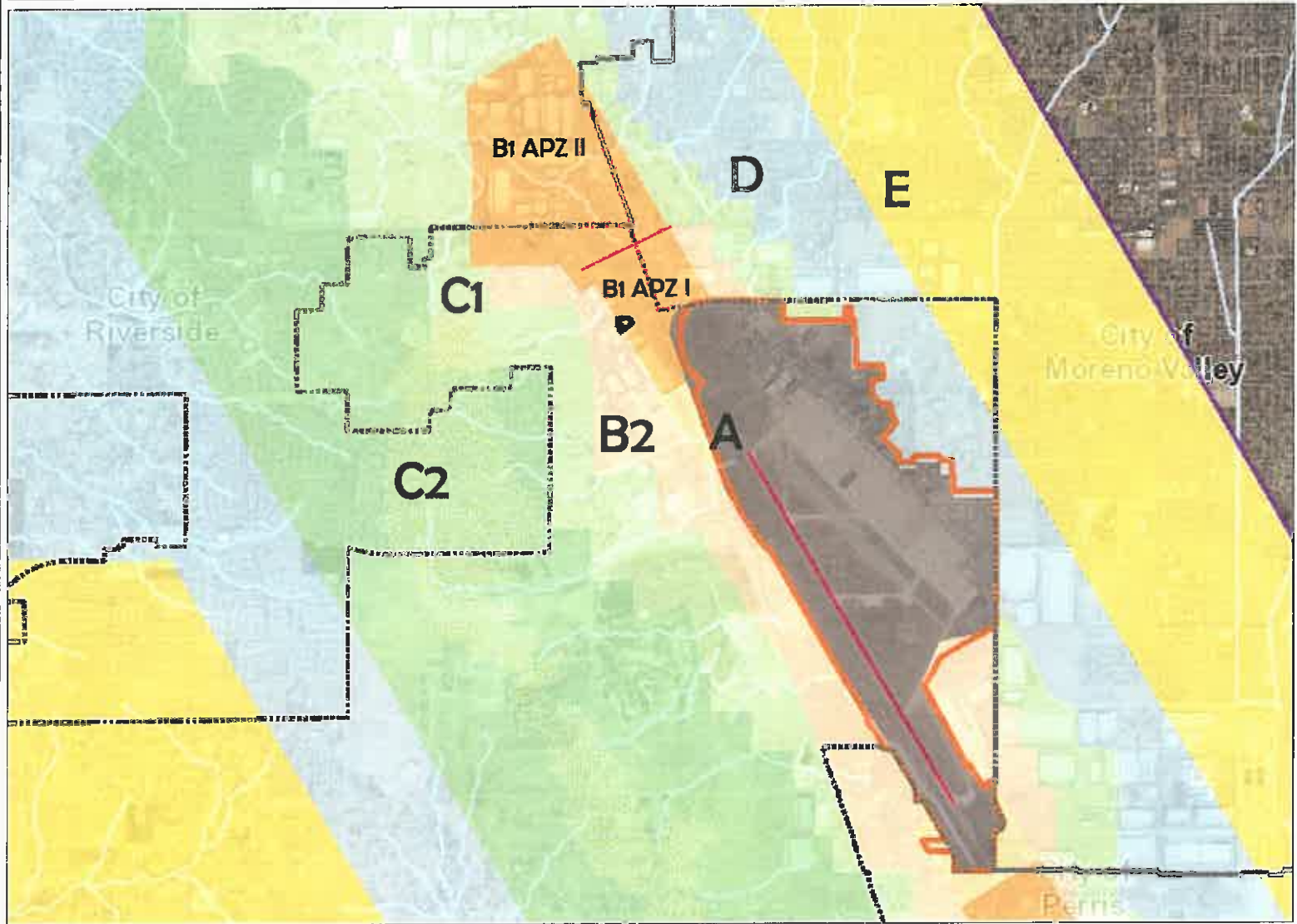
Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map MA-1

**Compatibility Map**  
March Air Reserve Base / Inland Port Airport

# Map My County Map



### Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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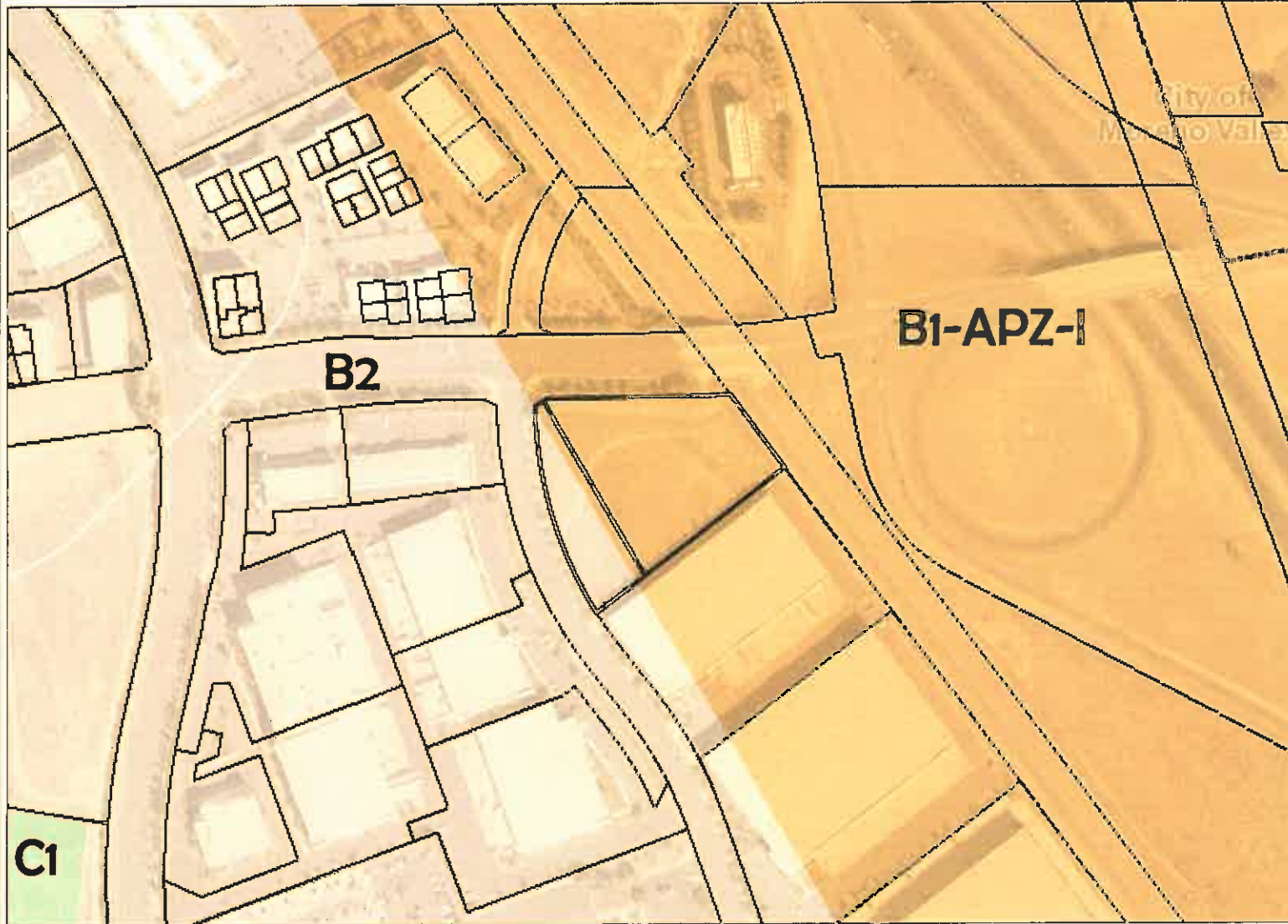


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### Notes

# Map My County Map



### Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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0 379 758 Feet

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### Notes

# Map My County Map

Los Angeles



San Diego

Tijuana

Mexico



## Legend

-  Parcels
-  Blueline Streams
-  City Areas
-  World Street Map



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## Notes

# Map My County Map



## Legend

- Blueline Streams
- City Areas
- World Street Map



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0 6 12,127 Feet  
064

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## Notes

# Map My County Map



## Legend

- Blueline Streams
- City Areas
- World Street Map



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0 1, 3,032 Feet  
516

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## Notes



# Map My County Map



## Legend

-  Parcels
-  Blueline Streams
-  City Areas
-  World Street Map



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## Notes

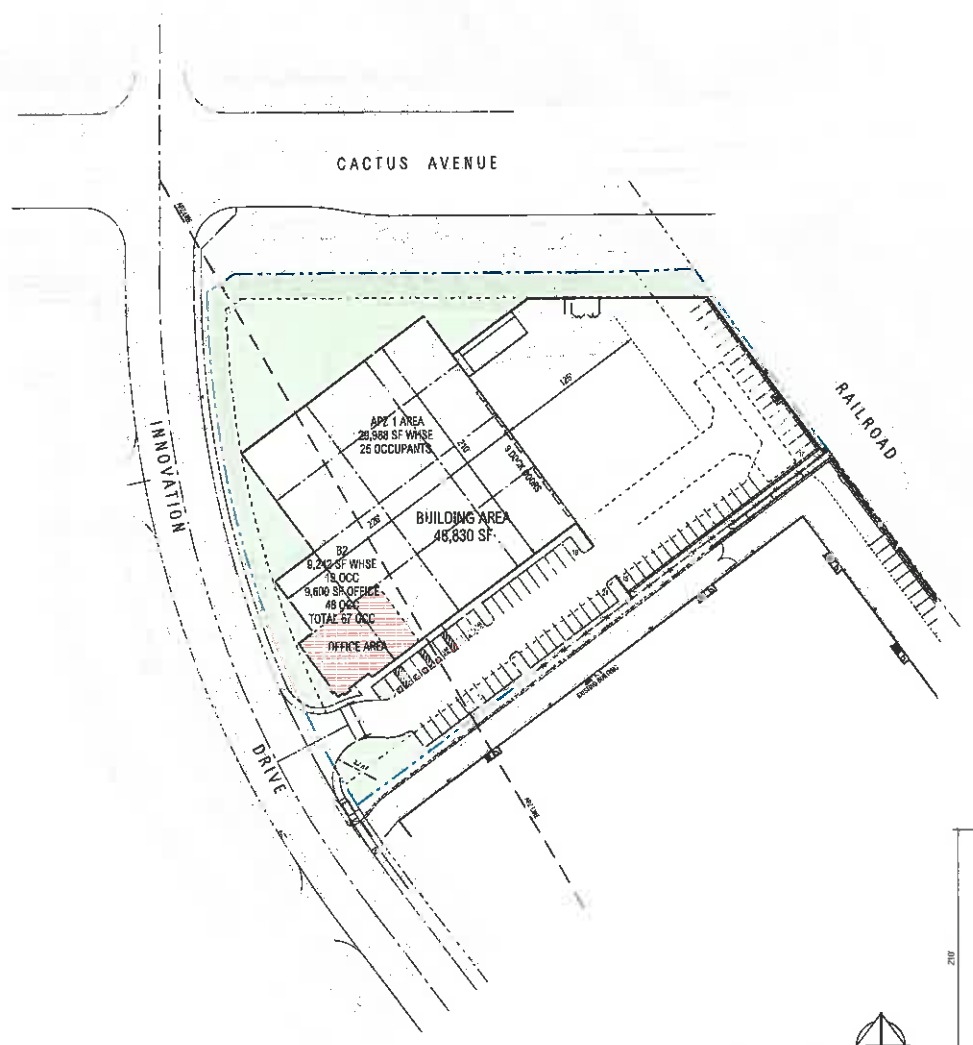
CONSULTANT

PROVISIONAL SDAS

**INNOVATION INDUSTRIAL**  
 MERIDIAN LOT 2  
 UNIT 1  
 14500 INNOVATION DRIVE  
 RIVERSIDE, CA 92518



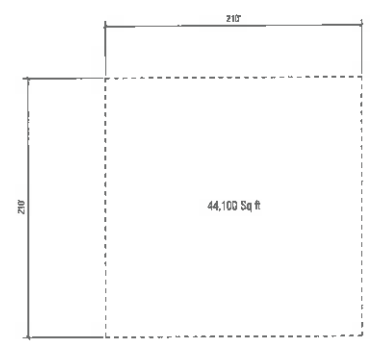
**VDA**  
 VINCENT VON DER AHE  
 28440 LA ALAMEDA, SUITE 270  
 MISSION VIEJO, CA 92691  
 949-348-9690  
 VINCENT VON DER AHE



**PROJECT DATA**

SITE AREA:	146,139 SF 3.22 AC
<b>APZ LAND AREAS:</b>	
INSIDE APZ ZONE 1	104,898 SF / 2.40 AC
OUTSIDE APZ ZONE	35,840 SF / 0.82 AC
TOTAL	140,738 SF / 3.22 AC
<b>APZ BUILDING AREAS:</b>	
INSIDE APZ ZONE 1 (MAX 25 OCC. PER AC)	28,988 SF
OUTSIDE APZ ZONE (MAX 120 OCC. PER AC)	19,842 SF
TOTAL	48,830 SF
<b>BUILDING AREA:</b>	
FIRST FLOOR OFFICE	4,800 SF
SECOND FLOOR OFFICE	4,800 SF
WAREHOUSE AREA	39,232 SF
TOTAL	48,830 SF
<b>COVERAGE:</b>	34.84 %
<b>PARKING REQUIRED:</b>	
5,800 SF OFFICE @ 3.3/1000	32 STALLS
0 - 30,000 SF @ 0/1000	0 STALLS
60,000 - 100,000 SF @ 0.5/3/1000	0 STALLS
TOTAL STALLS REQUIRED	71 STALLS
<b>AUTO PARKING PROVIDED:</b>	
STANDARD STALLS	67 STALLS
CARPOOL STALLS (5' x 10')	4 STALLS
FUTURE STALLS	0 STALLS
TOTAL STALLS PROVIDED	71 STALLS
<b>LANDSCAPE AREA PROVIDED:</b>	60 SF 0.0 %
<b>MINIMUM SETBACKS REQUIRED:</b>	
FRONT (A STREET SIDE YARD) BUILDING	27'
FRONT LANDSCAPE	15'
SIDE (ANTERIOR) BUILDING	0'
SIDE LANDSCAPE	5'
REAR BUILDING	27'
REAR LANDSCAPING	5'

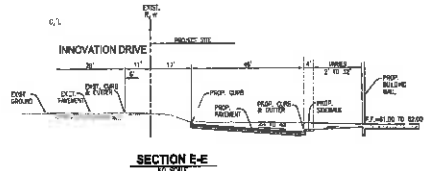
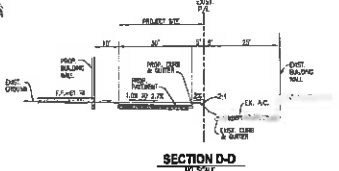
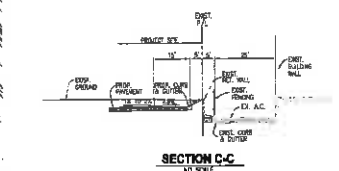
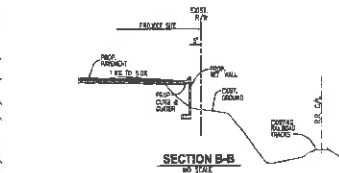
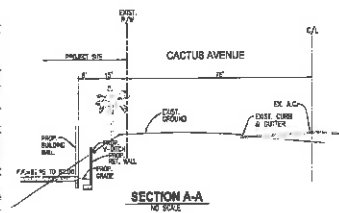
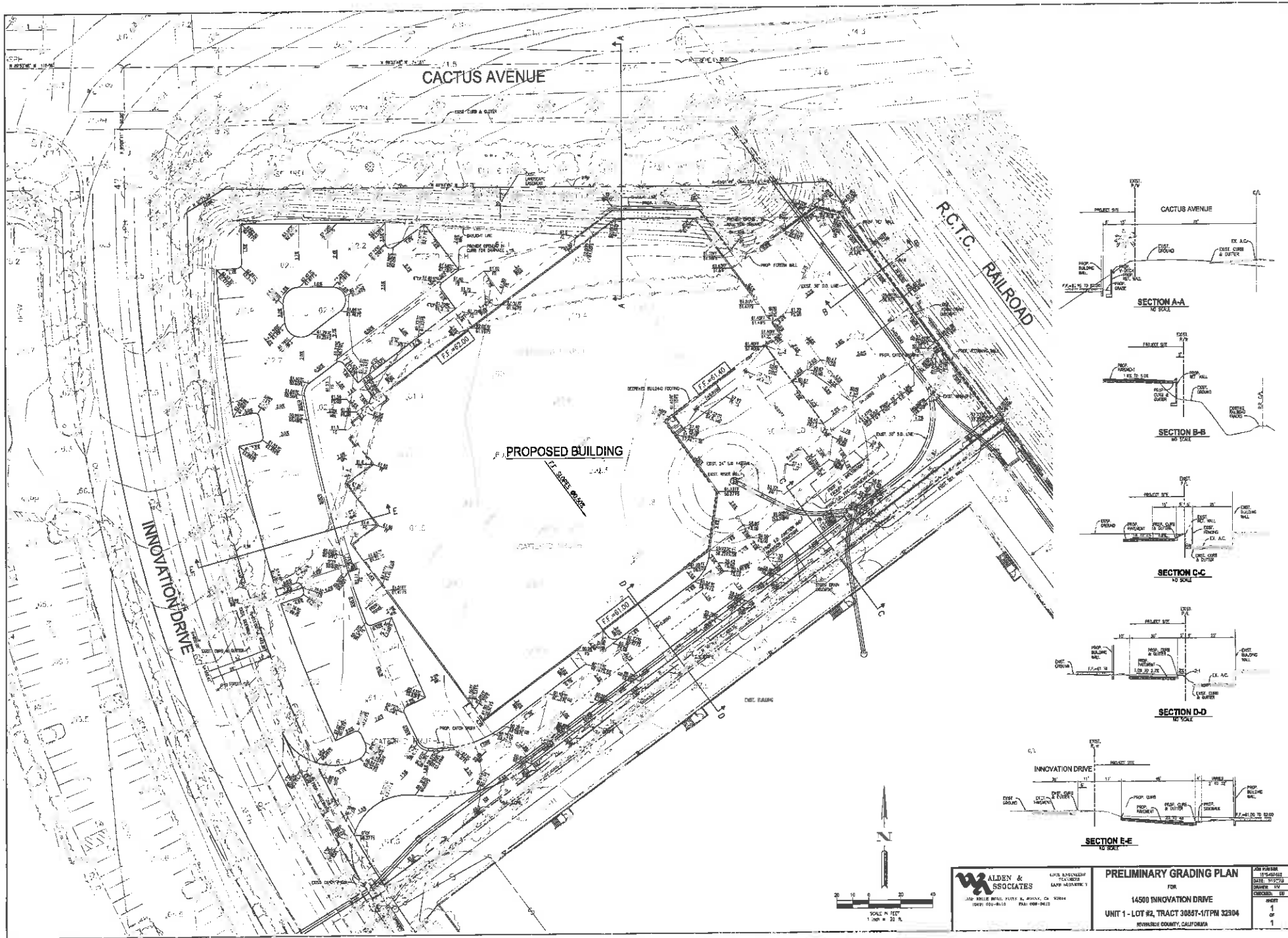
SITE PLAN LOT 2 UNIT 1  
 SCALE: 1" = 60'



CD		
ED		
PC		
SD		
SD	SD00	SCHEMATIC DESIGN
APP.	DATE	DESCRIPTION
REG PROJECT NO.	04621-01	
PROJ ARCHIT NO.	04621-01	
CD FILE NAME	04621-01-01-100	
DRAWN BY	MC	
CHECKED BY	CS	
COPYRIGHT	REGISTRATION OF ARCHITECTURE BOARD	
SHEET TITLE	SCHEMATIC SITE PLAN	







<p><b>WALDEN &amp; ASSOCIATES</b> LAND DEVELOPMENT LAND ARCHITECTURE</p> <p>1400 HILLS ROAD, SUITE 100, ANAHEIM, CA 92804 (714) 934-8110 FAX (714) 934-8113</p>	<p><b>PRELIMINARY GRADING PLAN</b></p> <p>FOR</p> <p><b>14500 INNOVATION DRIVE</b></p> <p><b>UNIT 1 - LOT #2, TRACT 30857-177PM 32904</b></p> <p>REVERSHADE COUNTY, CALIFORNIA</p>		<p>DATE: 11/20/23</p> <p>DRAWN BY: [Name]</p> <p>CHECKED BY: [Name]</p>
	<p>SCALE: 1" = 20'</p> <p>1" = 20' = 20' x 1"</p>		<p>SHEET</p> <p>1</p> <p>OF</p> <p>1</p>

**NOTICE OF PUBLIC HEARING**  
**RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**  
[www.rcaluc.org](http://www.rcaluc.org)

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893**. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The March Joint Powers Authority may hold hearings on this item and should be contacted on non-ALUC issues. For more information please contact March Joint Powers Authority Planner Mr. Jeffrey Smith at (951) 656-7000.

The proposed project application may be viewed at [www.rcaluc.org](http://www.rcaluc.org). Written comments may be submitted to the Riverside County ALUC by e-mail to [prull@rivco.org](mailto:prull@rivco.org), or by U.S. mail to Riverside County Administrative Center, 4080 Lemon Street, 14<sup>th</sup> Floor, Riverside, California 92501. Individuals with disabilities requiring reasonable modifications or accommodations, please telephone Barbara Santos at (951) 955-5132.

**PLACE OF HEARING:**                    **Riverside County Administration Center**  
                                                  **4080 Lemon Street, 1<sup>st</sup> Floor Board Chambers**  
                                                  **Riverside California**

**DATE OF HEARING:**                **August 13, 2020**

**TIME OF HEARING:**                **9:30 A.M.**

**Pursuant to Executive Order N-25-20, this meeting will be conducted by teleconference only. Information on how to participate in the hearing will be available on the ALUC website at [www.rcaluc.org](http://www.rcaluc.org)**

**CASE DESCRIPTION:**

**ZAP1393MA19 – Innovation Industrial Partners, LLC, Vincent Von Der Ahe (Representative: Kent Norton, MIG, Inc.) – March Joint Powers Authority Case No. PP19-03 (Plot Plan).** The applicant proposes to construct a 48,830 square foot industrial warehouse building with second floor mezzanines on 3.22 acres located on the southeast corner of Cactus Avenue and Innovation Drive (Airport Compatibility Zones B1-APZ-I and B2 of the March Air Reserve Base/Inland Port Airport Influence Area).



# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

<b>APPLICATION FOR MAJOR LAND USE ACTION REVIEW</b>			
ALUC CASE NUMBER: <u>ZAP1393MA19</u>		DATE SUBMITTED: <u>November 27, 2019</u>	
<b>APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION</b>			
<b>Applicant</b>	<u>Vincent Von der Ahe</u>	Phone Number	<u>949-348-9690</u>
<b>Mailing Address</b>	<u>VDA Real Estate Services</u>	Email	<u>vmv@vdaco.com</u>
	<u>26440 La Alameda, Suite 270</u>		
	<u>Mission Viejo, CA 92691</u>		
<b>Representative</b>	<u>MIG</u>	Phone Number	<u>951-787-9222</u>
<b>Mailing Address</b>	<u>1500 Iowa Avenue, Suite 110</u>	Email	<u>knorton@migcom.com</u>
	<u>Riverside, CA 92507</u>		
	<u>Attn: Kent Norton</u>		
<b>Property Owner</b>	<u>Vincent Von der Ahe</u>	Phone Number	<u>Same</u>
<b>Mailing Address</b>	<u>same as applicant (see above)</u>	Email	<u>Same</u>
<b>LOCAL JURISDICTION AGENCY</b>			
<b>Local Agency Name</b>	<u>March Joint Powers Authority</u>	Phone Number	<u>951-656-7000</u>
<b>Staff Contact</b>	<u>Jeff Smith, AICP, Senior Planner</u>	Email	<u>smith@marchjpa.com</u>
<b>Mailing Address</b>	<u>14205 Meridian Parkway, Suite 140</u>		
	<u>March Air Reserve Base, CA 92518</u>		
<b>Local Agency Project No</b>	<u>Plot Plan 19-03</u>		
	Case Type		
	<input type="checkbox"/> General Plan / Specific Plan Amendment		
	<input type="checkbox"/> Zoning Ordinance Amendment		
	<input type="checkbox"/> Subdivision Parcel Map / Tentative Tract		
	<input type="checkbox"/> Use Permit		
	<input checked="" type="checkbox"/> Site Plan Review/Plot Plan		
	<input type="checkbox"/> Other		
<b>PROJECT LOCATION</b>			
<i>Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways</i>			
<b>Street Address</b>	<u>14500 Innovation Drive</u>		
	<u>Riverside, CA 92518</u>		
<b>Assessor's Parcel No.</b>	<u>297-230-011 and -012</u>		
<b>Subdivision Name</b>	<u>Meridian Lot 2 Unit 1</u>	Gross Parcel Size	<u>3.22 acres</u>
<b>Lot Number</b>	<u>Lot 2, Tract Map 30857-1, Book 371</u>		Nearest Airport and distance from Airport
			<u>March ARB 4,686 ft</u>
<b>PROJECT DESCRIPTION</b>			
<i>If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed</i>			
<b>Existing Land Use (describe)</b>	<u>Site is vacant and rough graded land planned for industrial use within the Meridian Business Center Specific Plan (MBCSP).</u>		
	<u>Surrounding uses include a railroad line and the I-215 Freeway to the east, Cactus Avenue and existing warehouses to the north, Innovation Drive and existing warehouses to the west, and an existing warehouse to the south.</u>		

Proposed Land Use (describe)	One tilt-up industrial building with 45,900 SF of industrial use and 5,000 SF of office use (total area = 50,900 SF). The Meridian Business Center Specific Plan (MBCSP) designates the site for industrial use. The current site plan shows the building would occupy 36% of the site, landscaping 24%, and parking/travel ways 40% of the site. Minor revision to previous approval. See attached Project Description and Plans for additional details.		
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)	NA	
For Other Land Uses (See Appendix C)	Hours of Operation	No user selected but could be up to 24/7	
	Number of People on Site	117	Maximum Number average = 18.3 and max one-acre = 50
	Method of Calculation	ALUC Application Appendix C based on CBC standards (see attached Proj Desc).	
Height Data	Site Elevation (above mean sea level)	1562 feet (max finished floor) ft.	
	Height of buildings or structures (from the ground)	1,600 feet (max 38 feet) ft.	
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?		<input type="checkbox"/> Yes
	if yes, describe		<input checked="" type="checkbox"/> No
	_____		
	_____		

- A. **NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. **REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. **SUBMISSION PACKAGE:**
1. . . . . Completed ALUC Application Form
  1. . . . . ALUC fee payment
  1. . . . . Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
  1. . . . . Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
  1. . . . . CD with digital files of the plans (pdf)
  1. . . . . Vicinity Map (8.5x11)
  1. . . . . Detailed project description
  1. . . . . Local jurisdiction project transmittal
  3. . . . . Gummed address labels for applicant/representative/property owner/local jurisdiction planner
  3. . . . . Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. (Only required if the project is scheduled for a public hearing Commission meeting)



/

**COUNTY OF RIVERSIDE  
AIRPORT LAND USE COMMISSION**

**STAFF REPORT**

**AGENDA ITEM:** 3.2

**HEARING DATE:** August 13, 2020

**CASE NUMBER:** ZAP1099RI20 – The Motorcycle Company (Representative: Adkan Engineers)

**APPROVING JURISDICTION:** City of Riverside

**JURISDICTION CASE NO:** P20-0214 (General Plan Amendment), P20-0215 (Specific Plan Amendment), P20-0216 (Rezone), P20-0217 (Design Review)

**LAND USE PLAN:** 2005 Riverside Municipal Airport Land Use Compatibility Plan

**Airport Influence Area:** Riverside Municipal Airport

**Land Use Policy:** Airport Compatibility Zone E

**Noise Levels:** Outside the 55 CNEL contour

**MAJOR ISSUES:** None

**RECOMMENDATION:** Staff recommends that the General Plan Amendment, Specific Plan Amendment, and Rezone be found CONSISTENT with the 2005 Riverside Municipal Airport Land Use Compatibility Plan, and that the Design Review also be found CONDITIONALLY CONSISTENT, subject to the conditions included herein, and such additional conditions as may be required by the Federal Aviation Administration Obstruction Evaluation Service.

**PROJECT DESCRIPTION:** The applicant proposes to expand the existing Harley Davidson facility for motorcycle storage increasing the total building area from 35,794 square feet to 48,194 square feet (an increase of 12,400 square feet), to be constructed on new parcels increasing the total site acreage from 2.20 acres to 2.47 acres (an increase of 0.27 acres). The applicant also proposes to amend the newly added parcel's General Plan land use designation from Medium Density Residential to Commercial Regional Center, as well as a rezone from Residential R-1-7000 to Commercial General. The applicant also proposes to amend the Riverside Auto Center Specific Plan to include the new parcels into the specific plan boundaries.

**PROJECT LOCATION:** The site is located southerly of Indiana Avenue, westerly of Winstrom Street, northerly of Casablanca Street, and easterly of Jefferson Street, approximately 10,380 feet

southeasterly of the southeasterly terminus of Runway 9-27, and 13,206 feet southeasterly of the southerly terminus of Runway 16-34 at Riverside Municipal Airport.

**BACKGROUND:**

Non-Residential Land Use Intensity: Pursuant to the Riverside Municipal Airport Land Use Compatibility Plan, the project site is located within Compatibility Zone E, which does not restrict non-residential intensity.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zone E (hazards to flight).

Noise: The site is located outside the 55 dBA CNEL contour from Riverside Municipal Airport. Therefore, no special measures to mitigate aircraft noise are required at this location.

Part 77: While the site's distance from the southeasterly terminus of Runway 9-27 (at approximately 10,380 feet) is less than its distance from the southerly terminus of Runway 16-34 (approximately 13,206 feet), the latter point has a much lower elevation (747.5 feet above mean sea level [AMSL]) than the former point (815.8 feet AMSL). Therefore, Runway 16-34 is the applicable runway for determination of Part 77 noticing requirements at the project location. At a distance of 13,206 feet from Runway 16-34, structures with a top point elevation of 879.5 feet above mean sea level (or greater) would require notification to the Federal Aviation Administration Obstruction Evaluation Service (FAAOES). The project site elevation is 856 feet above mean sea level, and proposes a maximum building height of 24 feet, resulting in a top point elevation of 880 feet AMSL. Therefore, review of buildings by the FAAOES was required. The applicant has submitted Form 7460-1, and FAA OES has assigned Aeronautical Study No. 2020-AWP-7672-OE to this project. Its status is currently a "work in progress".

Open Area: The site is located within Airport Compatibility Zone E of the Riverside Municipal Airport Influence Area, which does not require projects to designate area as ALUC qualifying open area that could potentially serve as emergency landing areas.

General Plan Amendment/Specific Plan Amendment/Rezone: The proposed Commercial Regional Center General Plan land use designation, Commercial General zoning, and incorporation of the project site into the Riverside Auto Center Specific Plan boundary would be as, or more consistent, with the Compatibility Plan than the existing designation.

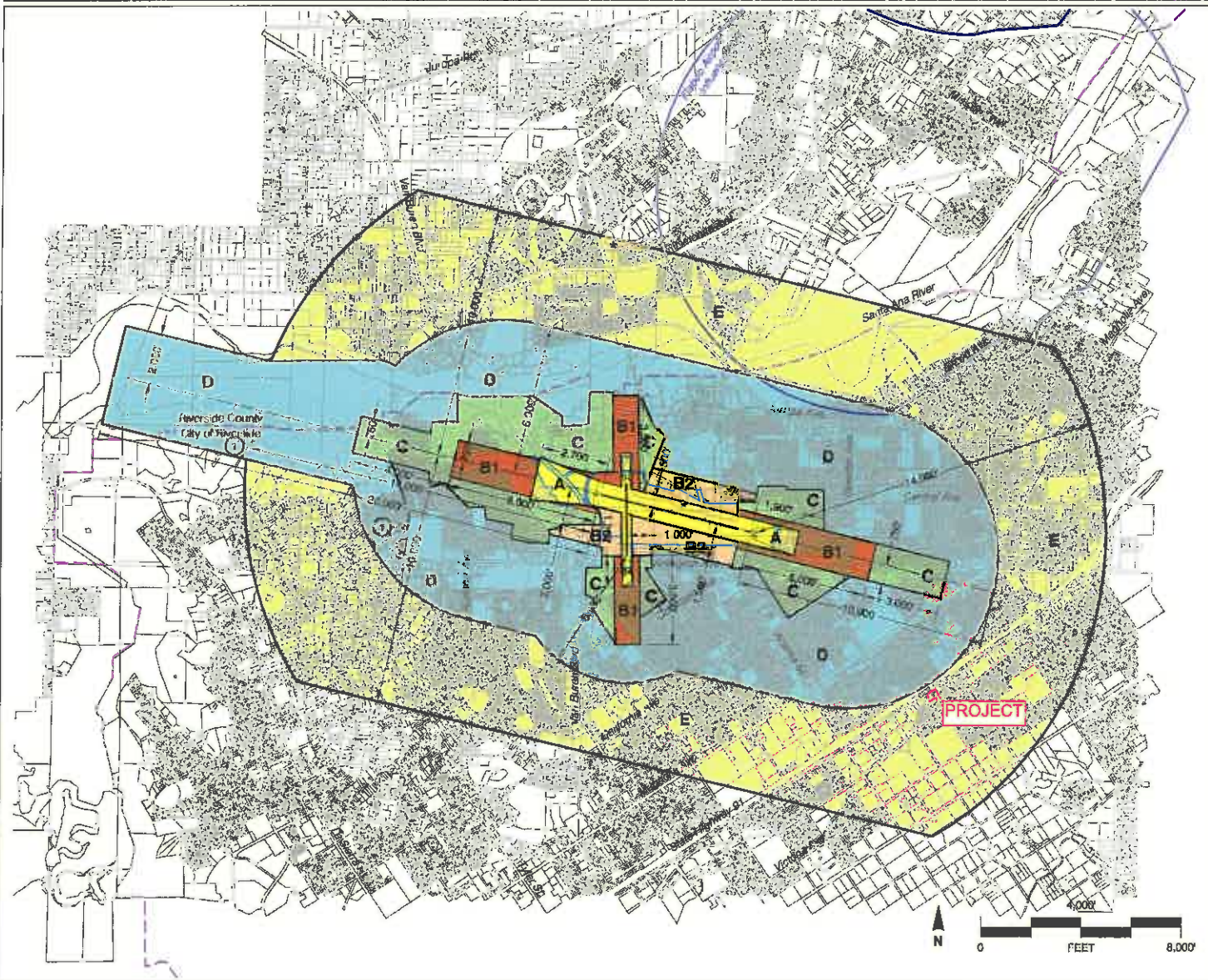
**CONDITIONS:**

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky.
2. The following uses shall be prohibited:

- (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached disclosure notice shall be provided to all potential purchasers, lessees, and/or tenants of the property.
4. Any new detention basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping.

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



**Legend**

**Compatibility Zones**

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E
- ★ Height Review Overlay Zone

**Boundary Lines**

- Airport Property Line
- City Limits

**Note**

Airport influence boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

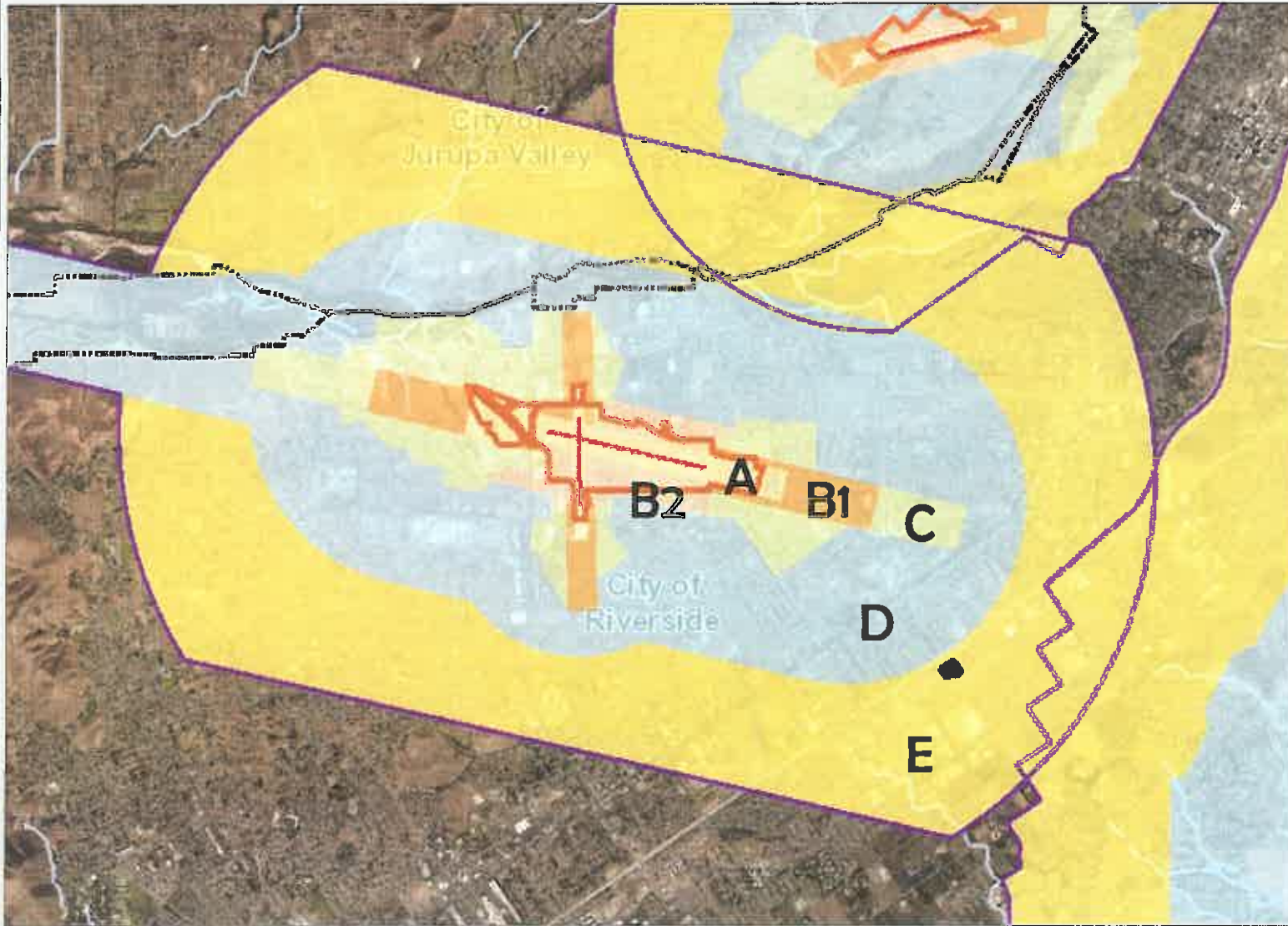
See Chapter 2, Table 2A for compatibility criteria associated with this map. See Section RI.2 for special exceptions to the Table 2A criteria.

**Riverside County**  
**Airport Land Use Commission**  
**Riverside County**  
**Airport Land Use Compatibility Plan**  
**Policy Document**  
*(Adopted March 2005)*

Map RI-1

**Compatibility Map**  
**Riverside Municipal Airport**

# Map My County Map



**Legend**

- Runways
- ▭ Airports
- ▭ Airport Influence Areas
- ▭ Airport Compatibility Zones
- ▭ OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6

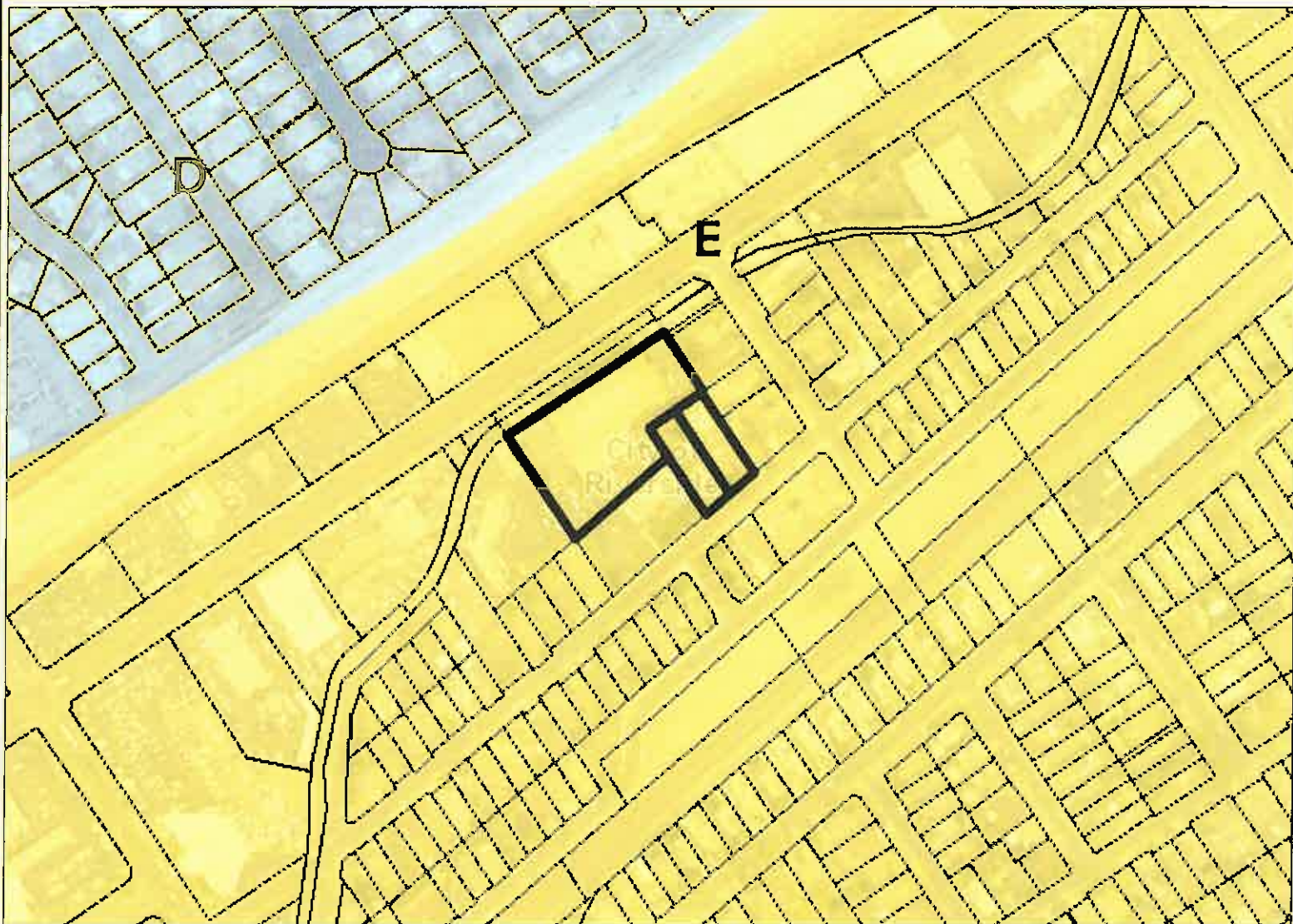


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**Notes**

# Map My County Map



**Legend**

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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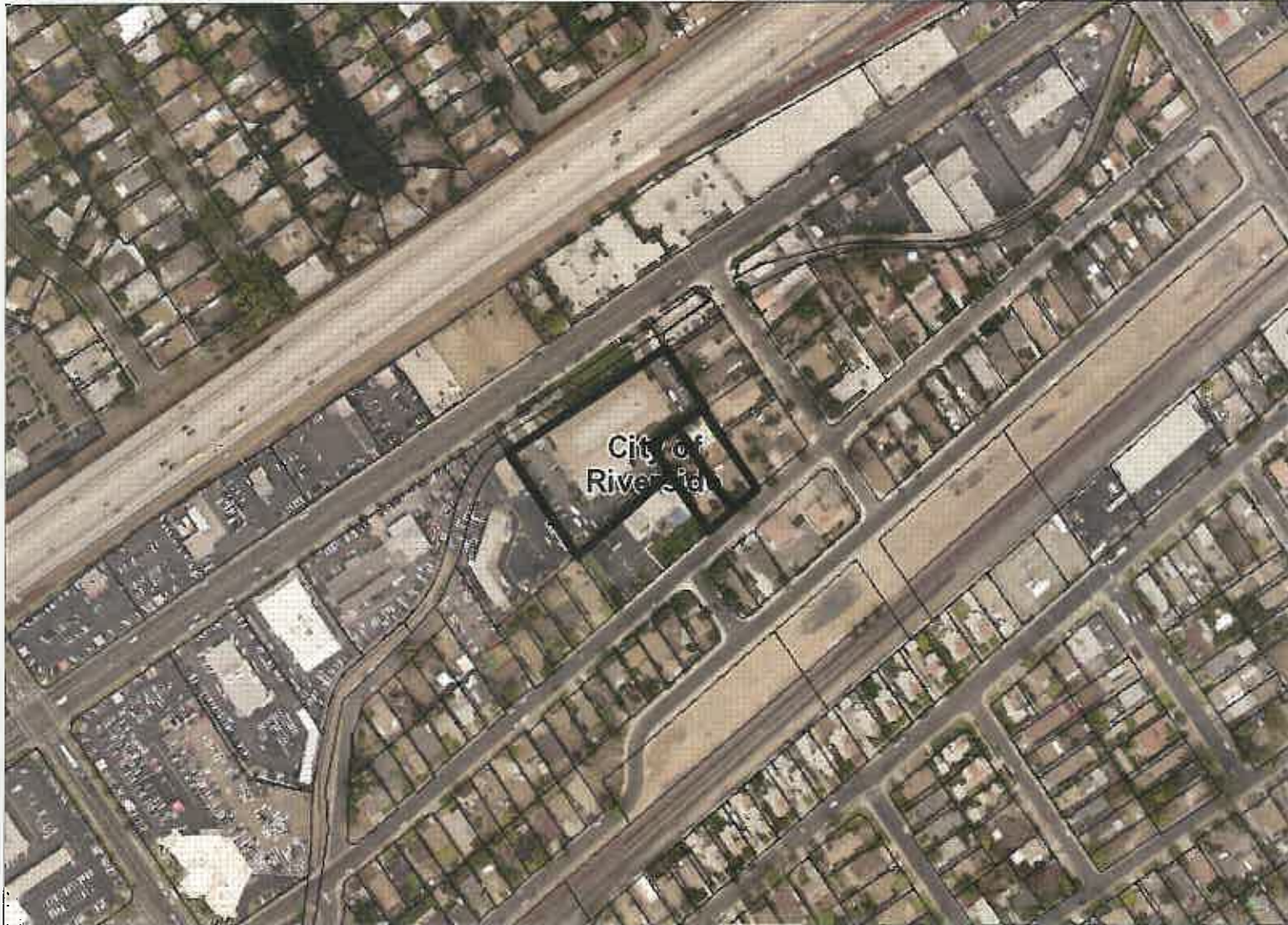


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**Notes**

# Map My County Map



## Legend

-  Parcels
-  Blueline Streams
-  City Areas
-  World Street Map



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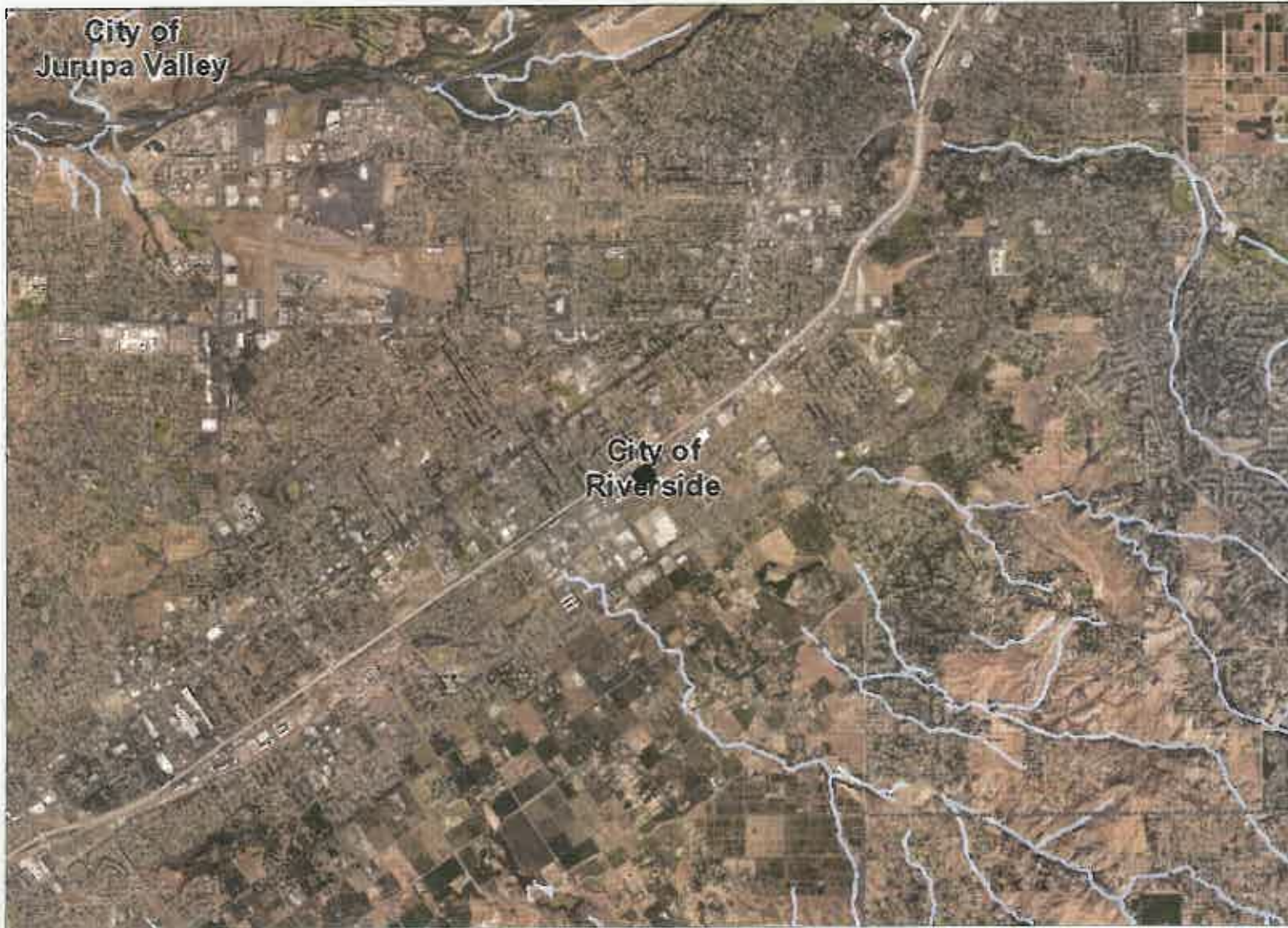
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## Notes



# Map My County Map



## Legend

- Blueline Streams
- City Areas
- World Street Map



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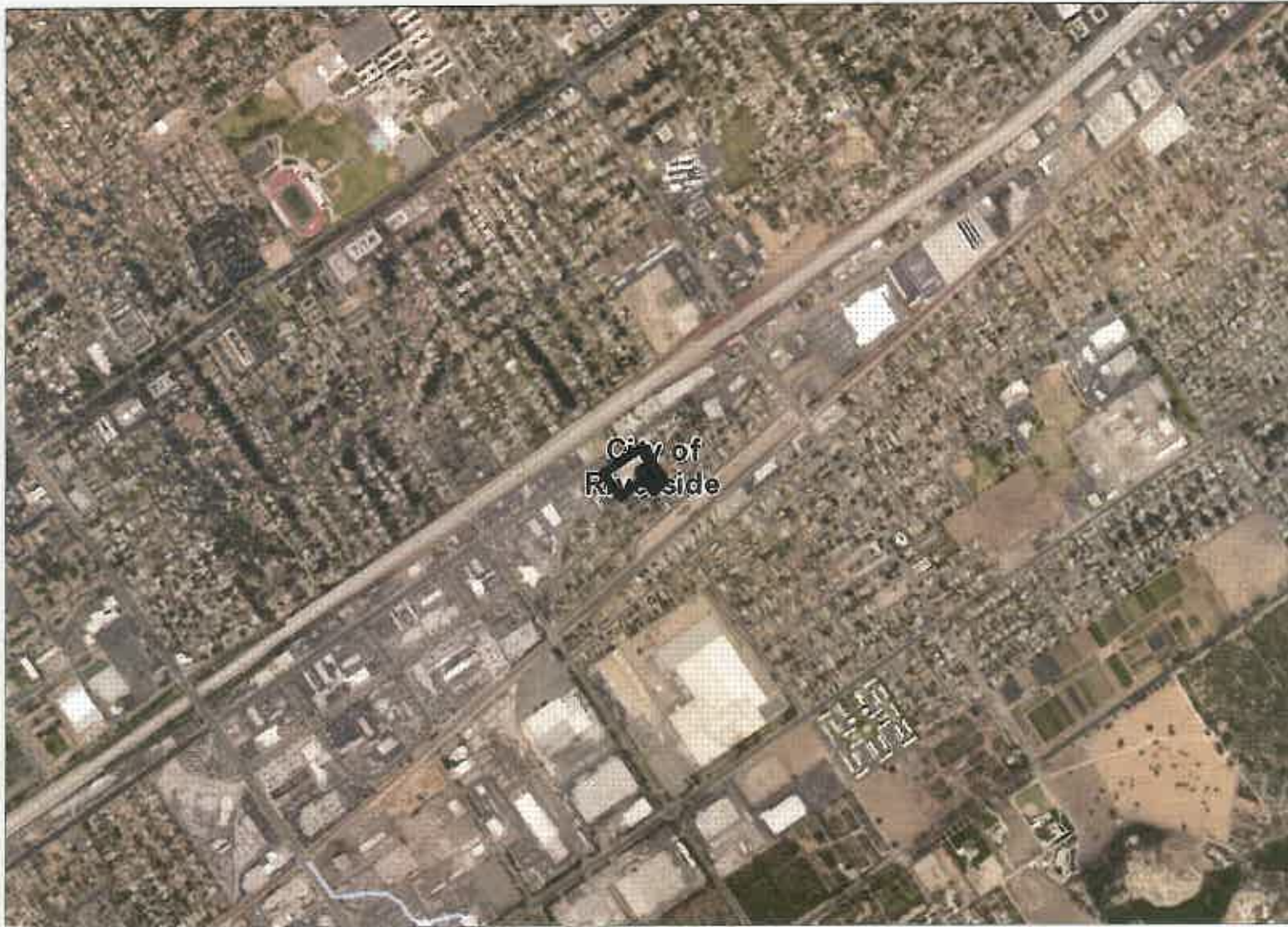


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## Notes

# Map My County Map



## Legend

- Blueline Streams
- City Areas
- World Street Map



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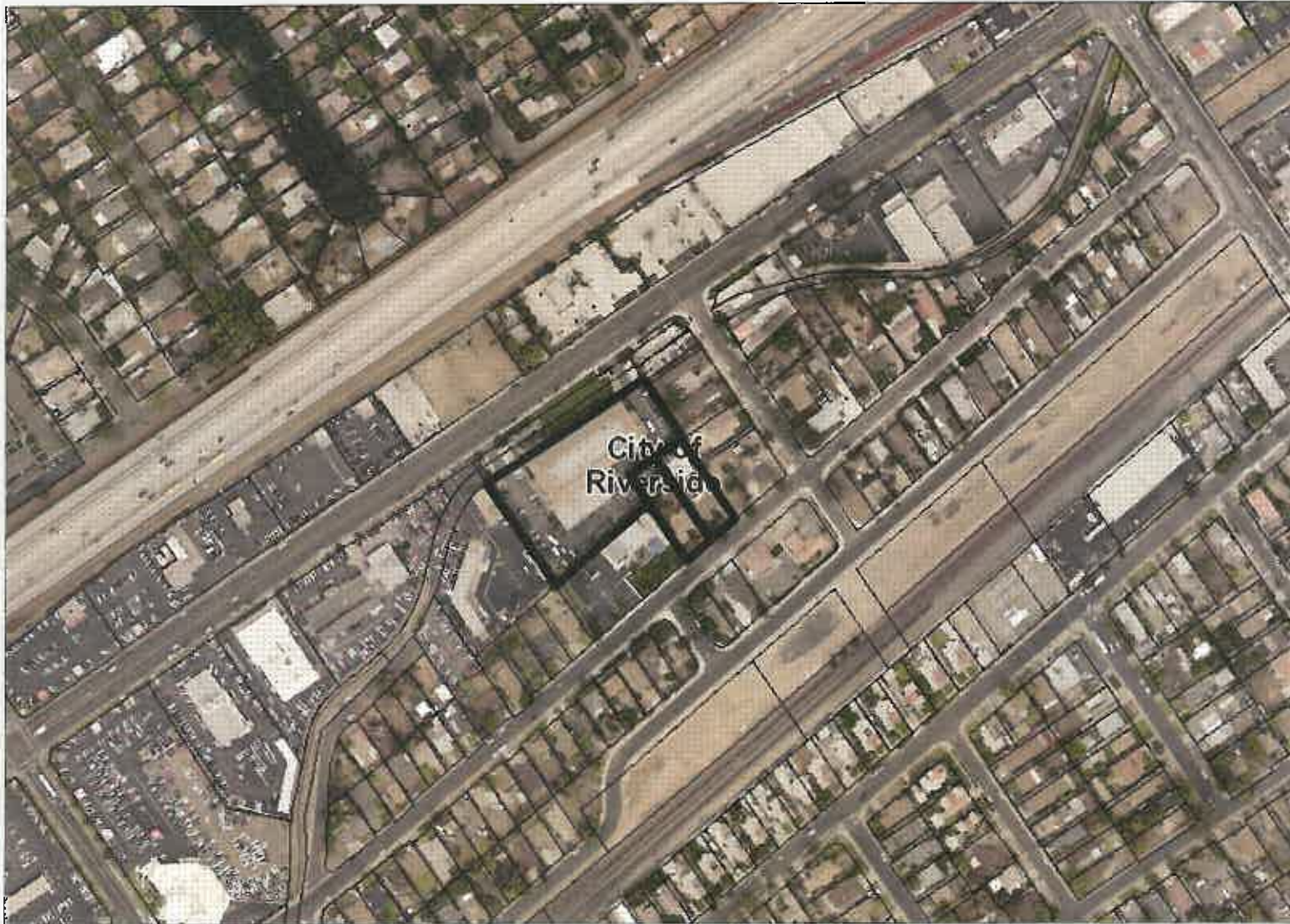
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## Notes

# Map My County Map



## Legend

-  Parcels
-  Blueline Streams
-  City Areas
-  World Street Map



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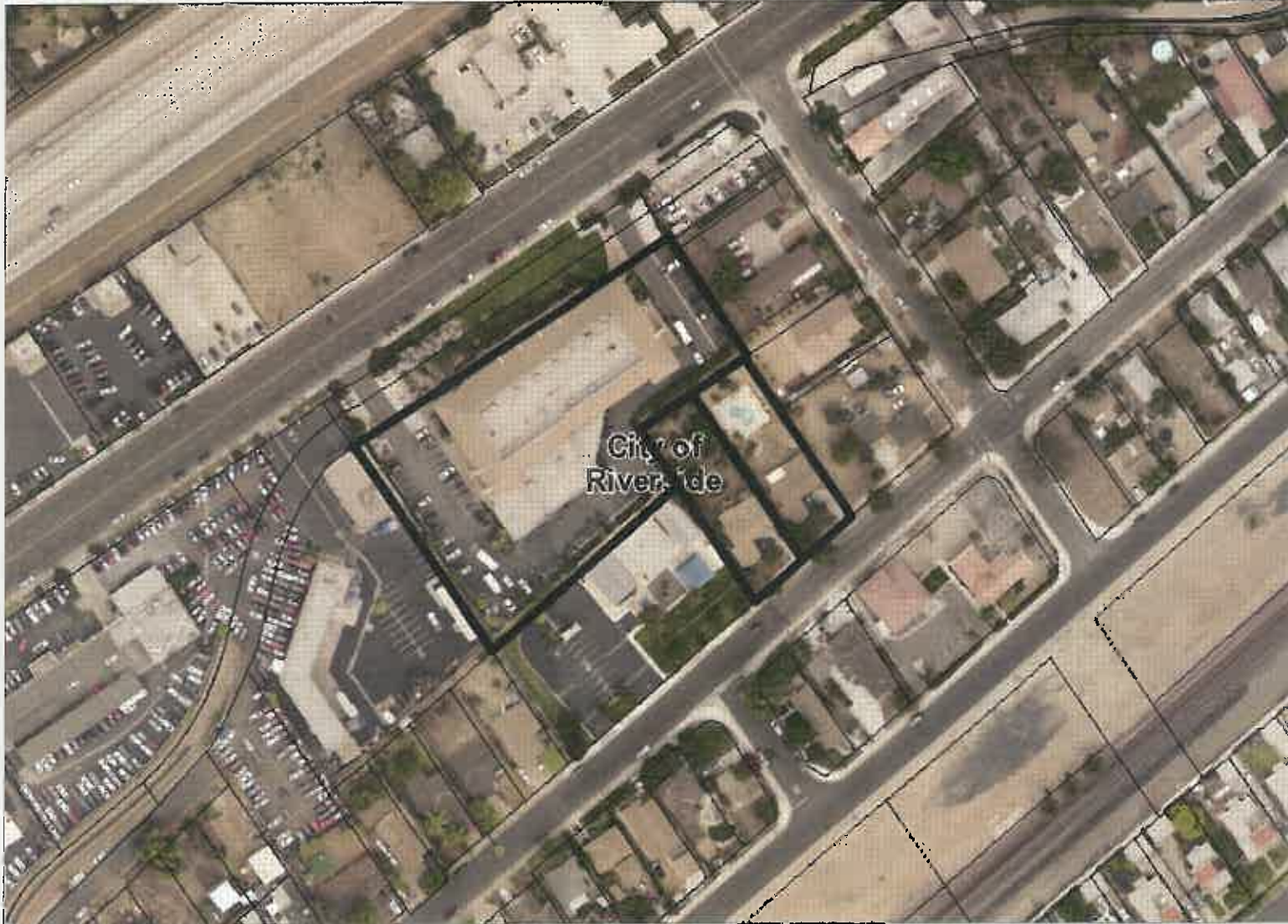
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## Notes

# Map My County Map



## Legend

-  Parcels
-  Blueline Streams
-  City Areas
-  World Street Map



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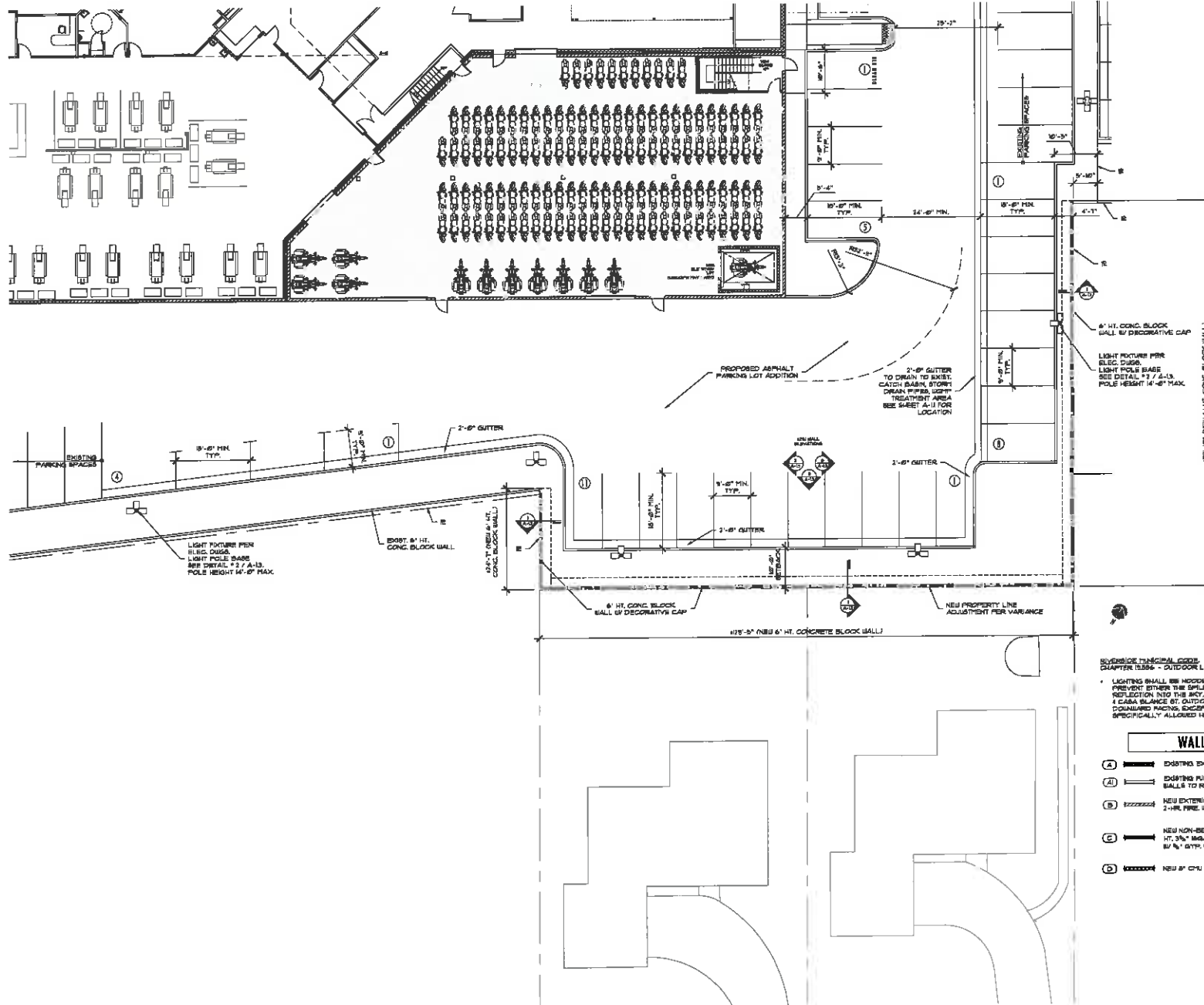
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## Notes









**designwerk**  
ARCHITECTURE INTERIOR DESIGN



PROJECT:  
TENANT IMPROVEMENT  
  
RIVERSIDE HARLEY-DAVIDSON  
7885 INDIANA AVE. • RIVERSIDE, CA 92504  
GLEN ESPINOZA • (951) 785-0100

**RIVERSIDE MUNICIPAL CODES**  
CHAPTER 18.886 - OUTDOOR LIGHTING

• LIGHTING SHALL BE HOODED OR SHIELDED SO AS TO PREVENT EITHER THE SPILLAGE OF LIGHTS OR REFLECTION INTO THE CITY, RESIDENCES, INDIANA AVE. & CALSA BLVD. BY OUTDOOR LIGHTING SHALL BE DISALLOWED EXCEPT AS MAY BE SPECIFICALLY ALLOWED HEREIN.

**WALL LEGEND**

- (A) ———— EXISTING EXTERIOR CMU WALL TO REMAIN.
- (AI) ———— EXISTING FULL HT. INTERIOR NON-BEARING WALLS TO REMAIN.
- (B) - - - - - NEW EXTERIOR 8" CMU WALL FULL HT. (MIN. 2'-0" FIRE WALL).
- (C) ———— NEW NON-BEARING INTERIOR WALL FULL HT. 3/4" MGA. MTL. FRAMING STUDS @ 16" O.C. @ 1/4" BAYS. SED. BOTH SIDES.
- (D) - - - - - NEW 8" CMU WALL FULL HT.

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PROJNO	NSAD
DATE	09/20/2020
DESIGN BY	PMU/ANP
SCALE	1/8" = 1'-0"
DESCRIPTION	DATE

SHEET TITLE  
**ENLARGED PROPOSED SITE PLAN**  
SHEET NUMBER  
**A-1.2**







PROJECT:  
TENANT IMPROVEMENT

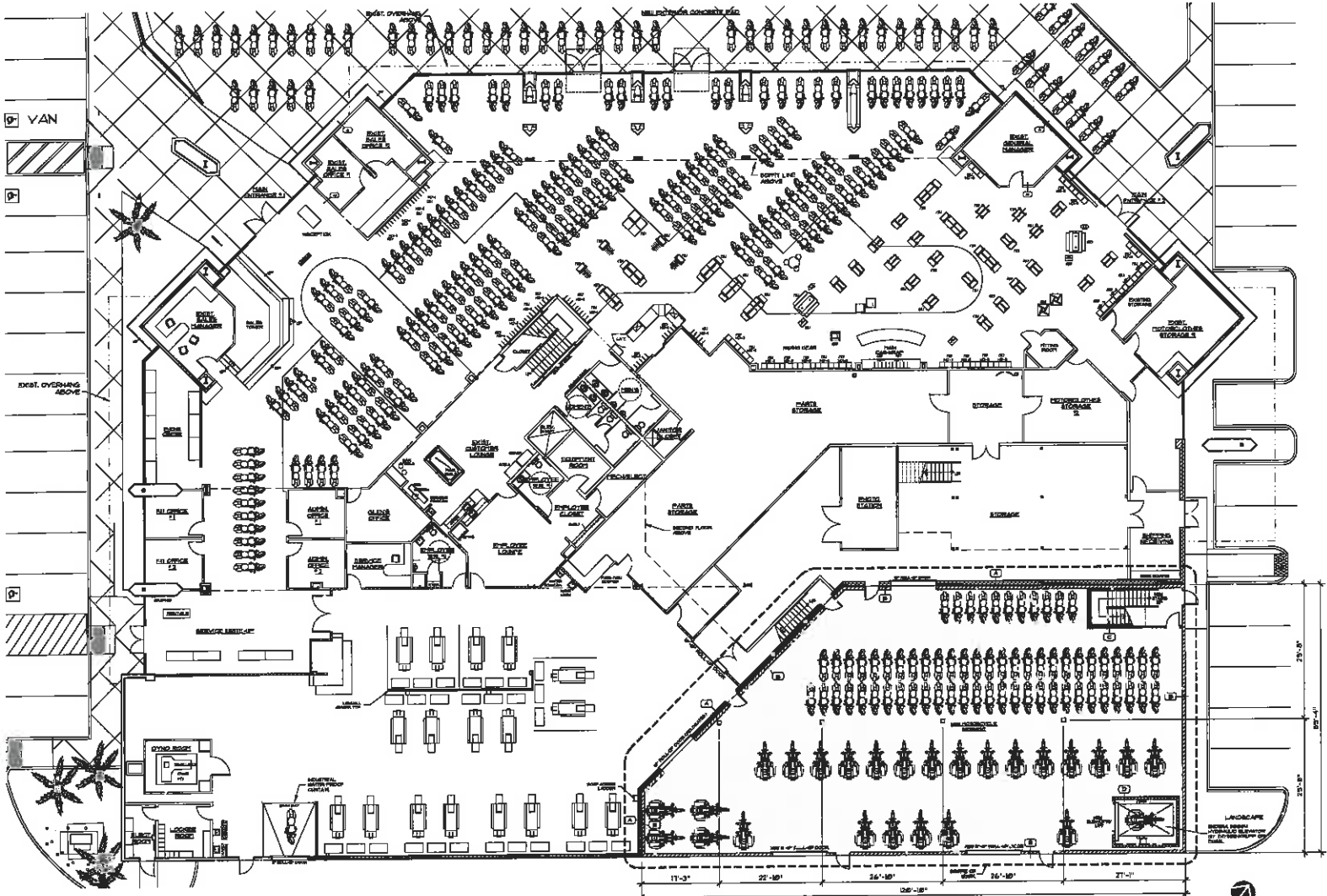


RIVERSIDE HARLEY-DAVIDSON  
788 INDIANA AVE. • RIVERSIDE, CA 92504  
GLEN ESPINOZA • (951) 785-6100

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NOT BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY  
ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING  
PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION  
RETRIEVAL SYSTEM, WITHOUT THE WRITTEN  
PERMISSION OF DAYWORK ARCHITECTS, INC.

PROJECT NO.	0848
DATE	03/26/2010
DRAWN BY	PL/HR/REP
SCALE	3/4" = 1'-0"
DESCRIPTION	DATE

DATE TITLE  
**PROPOSED FLOOR PLAN  
(FIRST FLOOR)**  
SHEET NUMBER **A-2.0**



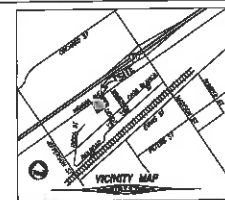
<b>WALL LEGEND</b>		<b>FIRST FLOOR AREA TABULATION</b>	
(A)	EXISTING EXTERIOR CMU WALL TO REMAIN.	EXISTING SF	31,501 SF
(A1)	EXISTING FULL HT. INTERIOR NON-BEARING WALLS TO REMAIN.	NEW ADDITION SF	6,280 SF
(B)	NEW EXTERIOR 8" CMU WALL FULL HT. (MIN. 2-HR. FIRE WALL). SEE DETAIL.	TOTAL 1ST FLOOR SF	38,281 SF
(C)	NEW NON-BEARING INTERIOR WALL FULL HT. 5/8" SIGA. HTL. FRAME STUDS @ 16" O.C. @ 1/2" GYP. SO. BOTH SIDES. SEE DETAIL.		
(D)	NEW 8" CMU WALL FULL HT.		







# CONCEPTUAL GRADE PLAN THE MOTORCYCLE COMPANY RIVERSIDE FACILITY



SECTION 4 & 9, T. 3S, R. 5W

**OWNER/APPLICANT**  
THE MOTORCYCLE COMPANY  
7833 HIGHWAY AVE.  
RIVERSIDE, CA 92504  
TEL: (951) 785-0100

**CIVIL ENGINEER**  
ADKAN ENGINEERS  
8875 AIRPORT DRIVE  
RIVERSIDE, CA 92504  
TEL: (951) 988-0247

**ZONING/LAND USE**  
EXISTING ZONING: O2  
PROPOSED ZONING: O2/PS  
EXISTING LANDUSE: O2/PS  
PROPOSED LANDUSE: O2/PS

**ZONING/LAND USE AREA BEING OBTAINED IN LOT LINE ADJUSTMENT**  
EXISTING ZONING: R-1-7000  
PROPOSED ZONING: R-1-7000  
EXISTING LANDUSE: R-1-7000  
PROPOSED LANDUSE: O2/PS

**ASSESSOR'S PARCEL NUMBER**  
210-242-021, 210-242-003, 210-242-006

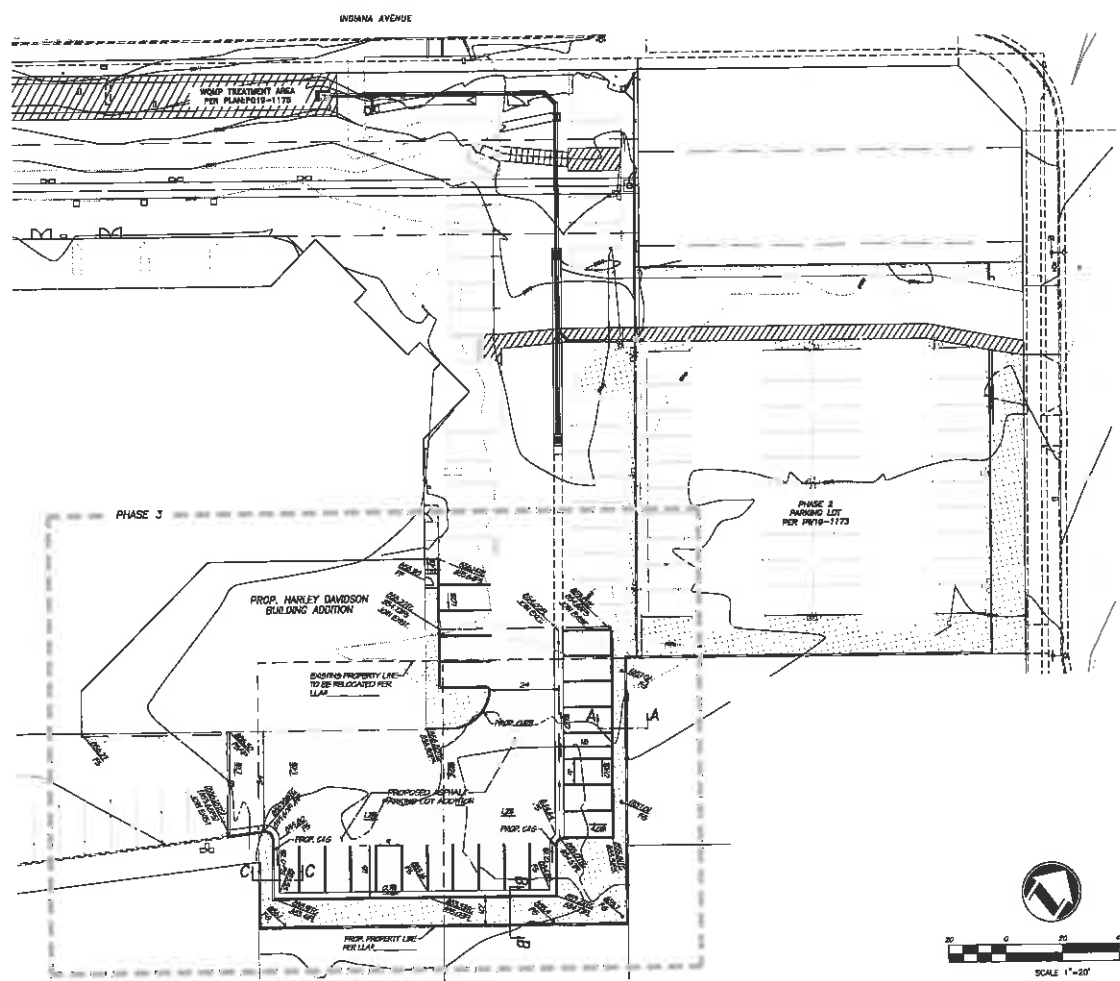
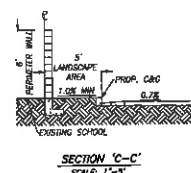
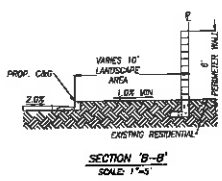
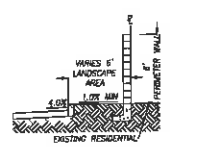
**PROPOSED PARKING STALLS**  
STANDARD PARKING STALLS.....19 OR

**PROJECT AREA**  
0.48 ACRES

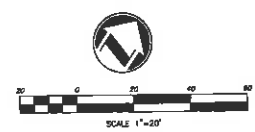
**PROJECT AREA**  
0.48 ACRES

**PROJECT DESCRIPTION**

THIS PROJECT PROPOSES A PARKING LOT EXPANSION ON WESTMONT PARCEL, DESCRIBED AS FOLLOWS:  
COMMENCING AT THE MOST NORTHERLY CORNER OF SAID LOT 2; THENCE SOUTH 25 DEGREES 00'00" WEST ALONG THE NORTHWESTERLY LINE OF SAID LOT 2A A DISTANCE OF 142.88 FEET TO A POINT THEREIN; THENCE SOUTH 11 DEGREES 00'00" EAST A DISTANCE OF 137.00 FEET; THENCE NORTH 25 DEGREES 00'00" EAST A DISTANCE OF 134.99 FEET TO THE MOST SOUTHWESTLY CORNER OF SAID PARCEL OF LAND CONVEYED TO THE CITY OF RIVERSIDE BY ORD. PROPOSED MAY 29, 1954 IN BOOK 1539, PAGE 378 OF PUBLIC RECORDS OF RIVERSIDE COUNTY, CALIFORNIA; THENCE NORTH 33 DEGREES 30'00" WEST ALONG THE SOUTHWESTERLY LINE OF SAID PARCEL 30 CONVEYED, A DISTANCE OF 137.00 FEET TO A POINT THEREIN; THENCE SOUTH 55 DEGREES 00'00" WEST A DISTANCE OF 134.88 FEET TO THE TRUE POINT OF BEGINNING.



- LEGEND**
- WWP TREATMENT AREA
  - PROP. CONCRETE
  - PROP. ASPHALT
  - PROP. LANDSCAPING
  - SUMP INLET
  - PROP. STORMING
  - PROP. W/OUTLET IRON FENCE
  - PROP. PERIMETER WALL
  - PROP. LIGHTING
  - FG FINISH GROUND
  - FS FINISH SURFACE
  - INV INVERT ELEVATION
  - OS ONSIDE SWATH
  - LS PROP. LANDSCAPE
  - R/W RIGHT OF WAY
  - PL PROPERTY LINE
  - CWS CURB AND GUTTER
  - TC TOP OF CURB
  - PROP. PROPOSED



**CONCEPTUAL GRADING PLAN  
THE MOTORCYCLE COMPANY PHASE 3  
BUILDING AND PARKING LOT ADDITION ON-SITE**

PREPARED BY  
**adkan ENGINEERS**  
Civil Engineering - Surveying - Planning  
1578 Avenue Crown, (RIVERSIDE), CA 92504  
TEL: (951) 988-0241, FAX: (951) 988-0249  
205 The Motorcycle Company, W.D. 5200  
SCALE: 1"=25' DATE: 12/2019

# DEMOLITION PLAN

EXIST. HARLEY DAVIDSON  
DEALERSHIP

DEMO 5' OF CURB  
AND JOIN PHASE 3  
IMPROVEMENTS PER  
SEPARATE PLAN

REMOVE AND DISPOSE OF  
EXISTING IMPROVEMENTS

REMOVE 192'± OF EXISTING CURB

EXISTING PROPERTY LINE  
TO BE RELOCATED PER  
LLA#

REMOVE AND DISPOSE  
OF EXISTING FENCE

REMOVE AND DISPOSE  
OF EXISTING FENCE

REMOVE 192'± OF EXISTING  
CURB

DEMO CONCRETE SLAB AND  
FILL IN EXISTING POOL

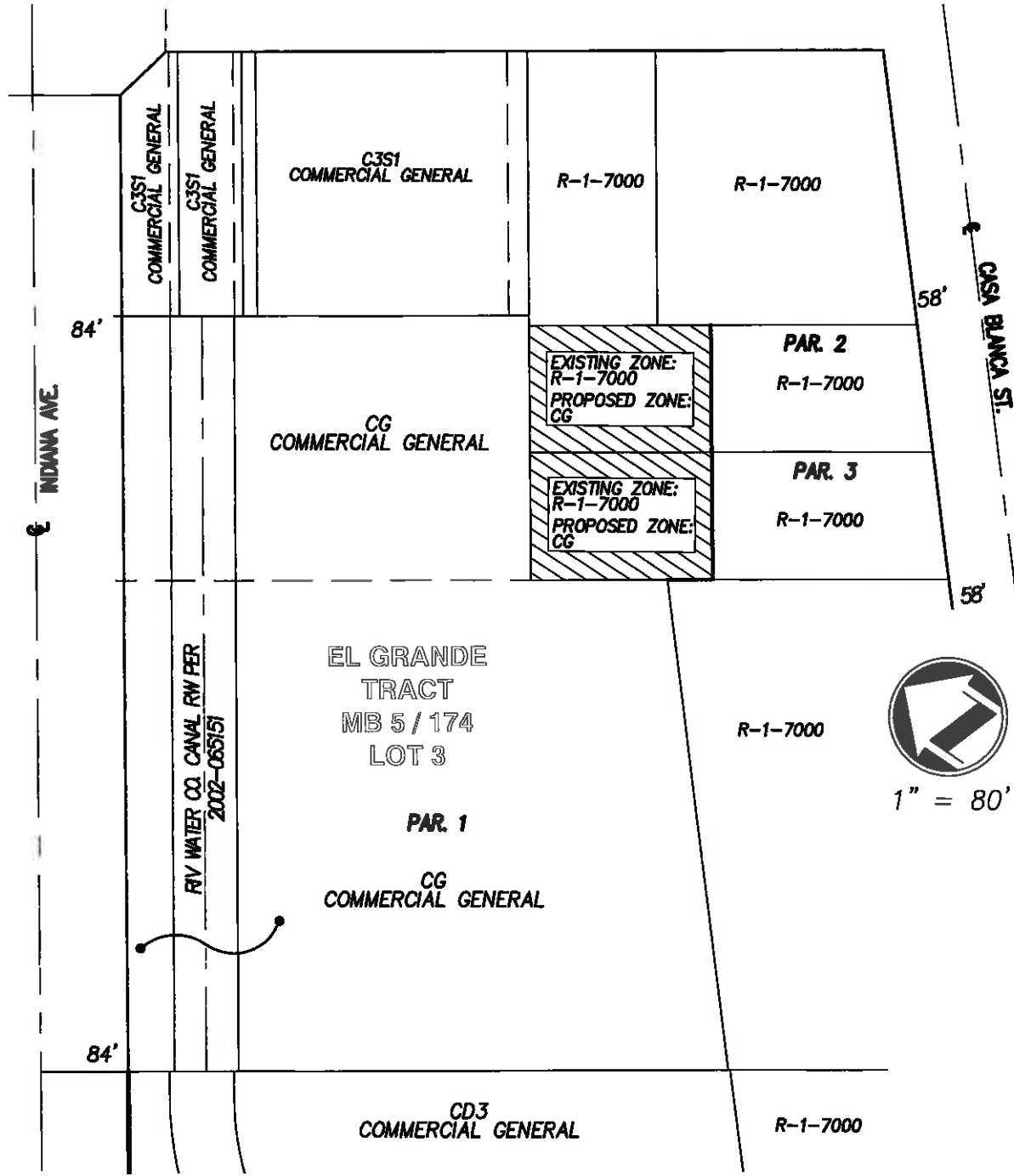
PROP. PROPERTY LINE  
PER LLA#

DEMOLITION PLAN  
THE MOTORCYCLE  
COMPANY PHASE 3  
BUILDING AND PARKING  
LOT ADDITION

Prepared by  
**adkan**  
**ENGINEERS**  
Civil Engineering • Surveying • Planning  
1879 Airport Drive, Riverside, CA 92504  
TEL: (951) 885-0241, (951) 885-0248  
FOR: The Motorcycle Company, W.D. #206  
SCALE: 1"=30' DATE: 12/23/19

# CHANGE OF ZONE EXHIBIT

SHEET 1 OF 1

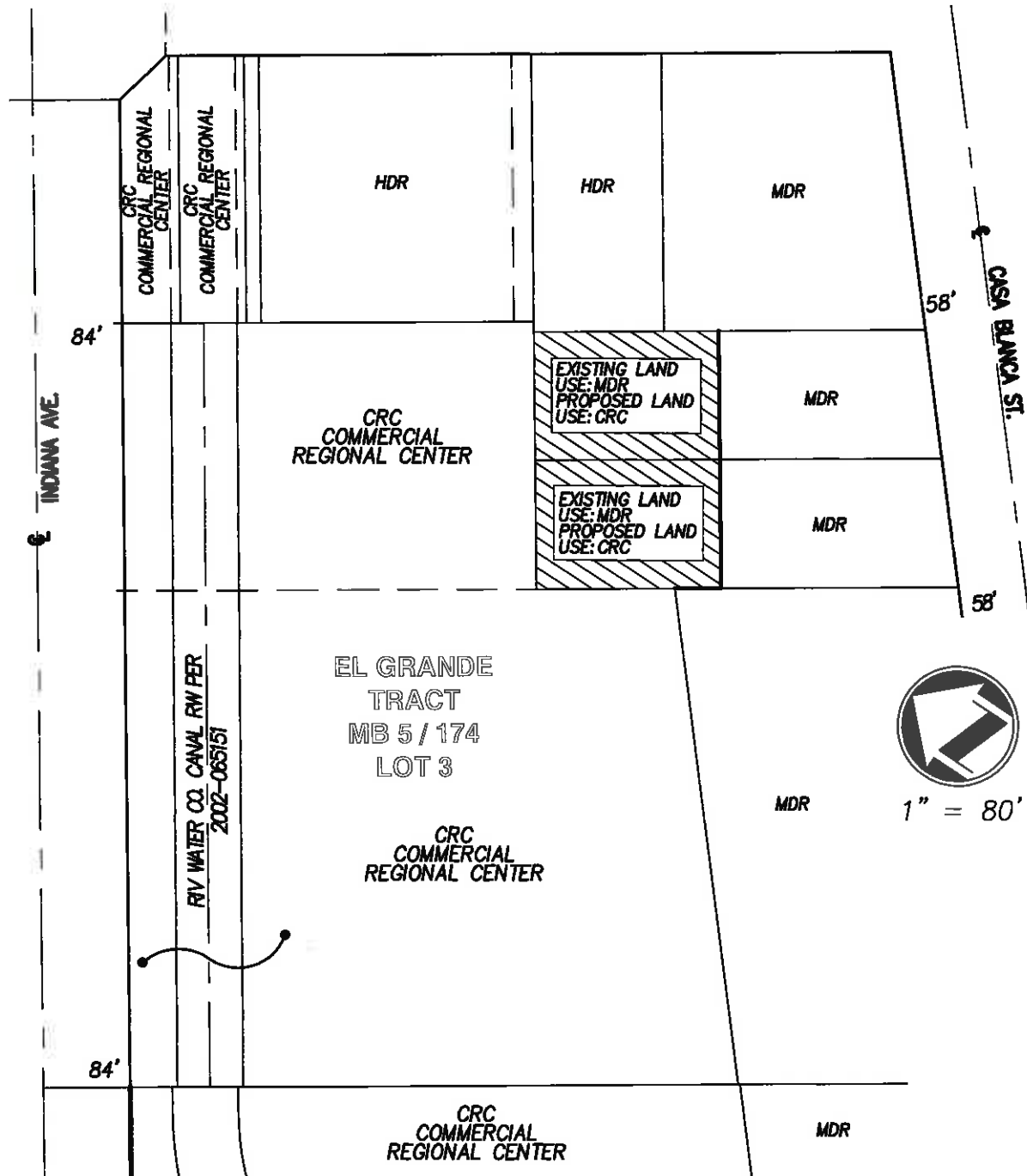


<p>PLAT PREPARED BY:</p> <p><b>adkan ENGINEERS</b></p> <p><i>Civil Engineering · Surveying · Planning</i></p> <p>6879 Airport Drive, Riverside, CA 92504 Tel: (951) 688-0241 · Fax: (951) 688-0599</p>	<p>JOB NO. 9206</p>	<p>DATE: 4-07-2020</p>	<p>CLIENT: THE MOTORCYCLE COMPANY</p>
	<p>APPROVED BY:</p> <p>_____ Michael R Brendecke, PLS</p>		<p>CHANGE OF ZONE EXHIBIT</p>



# GENERAL PLAN AMMENDMENT

SHEET 1 OF 1



PLAN PREPARED BY: <b>adkan ENGINEERS</b> <i>Civil Engineering • Surveying • Planning</i> 6879 Airport Drive, Riverside, CA 92504 Tel: (951) 688-0241 • Fax: (951) 688-0599	JOB NO. 9206	DATE: 4-07-2020	CLIENT: THE MOTORCYCLE COMPANY
	APPROVED BY:  _____ Michael R Brendecke, PLS		GENERAL PLAN AMMENDMENT EXHIBIT

# ***RIVERSIDE AUTO CENTER SPECIFIC PLAN***

Originally Adopted on October 23, 1990  
Resolution 17614  
GP-003-901

*Edited to include all Specific Plan Amendments as of November 2007  
by the City of Riverside Planning Department*

## **SPECIFIC PLAN AMENDMENTS**

<b>Case</b>	<b>Adoption Date</b>	<b>Resolution No.</b>
SP-005-912		
SP-006-912		
SP-003-989	July 27, 1999	19543
SP-001-990	November 23, 1999	19602
SP-002-990 A SP-002-990 B SP-004-990	March 28, 2000	19663
SP-005-001	February 6, 2001	19835
SP-005-001 Corrected	October 10, 2006	21262
P03-0401	October 14, 2004	20553
P04-0178	November 20, 2007	21536
P20-XXXX	June 15, 2020	XXXX

Figure 1 — Specific Plan Area

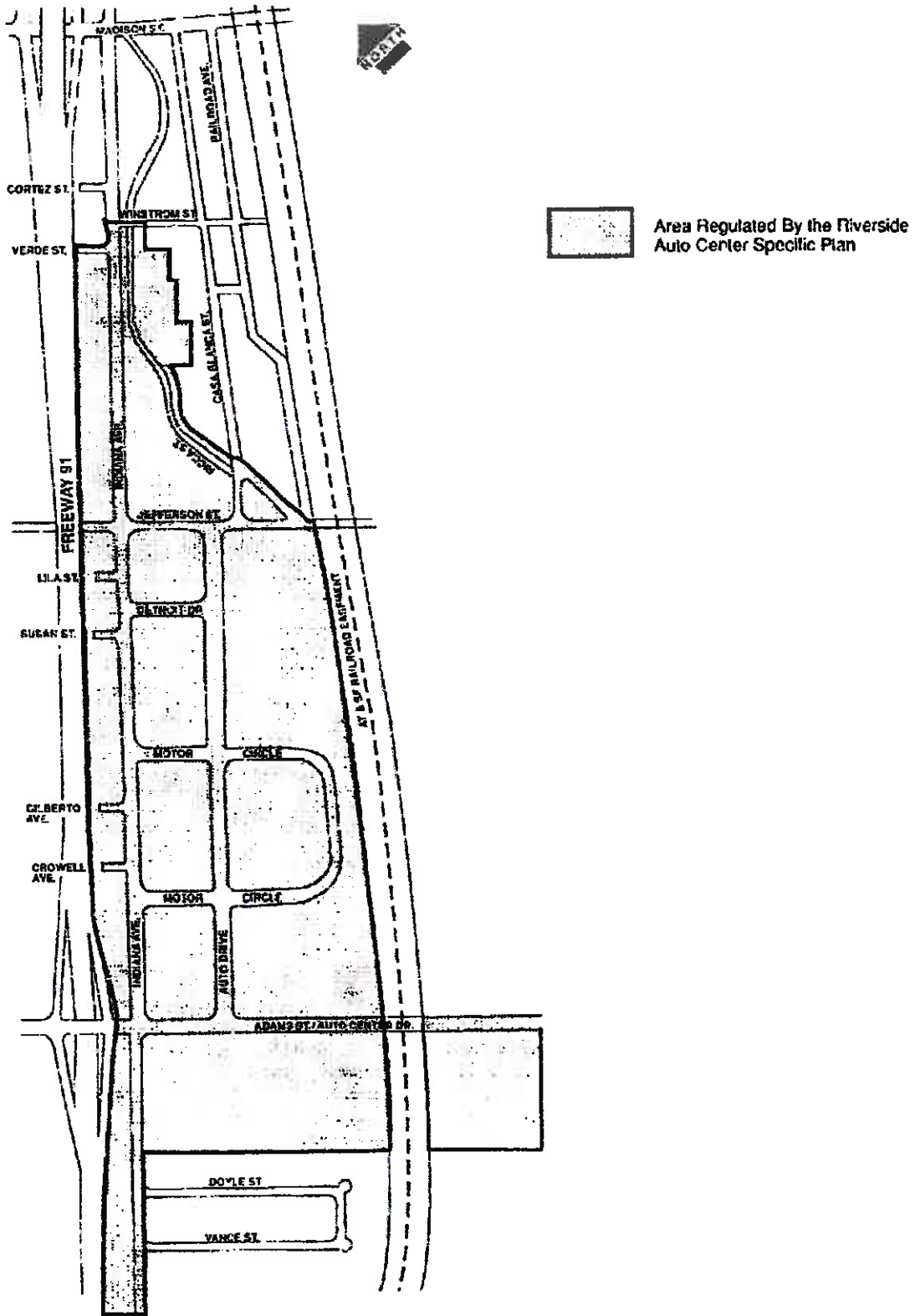
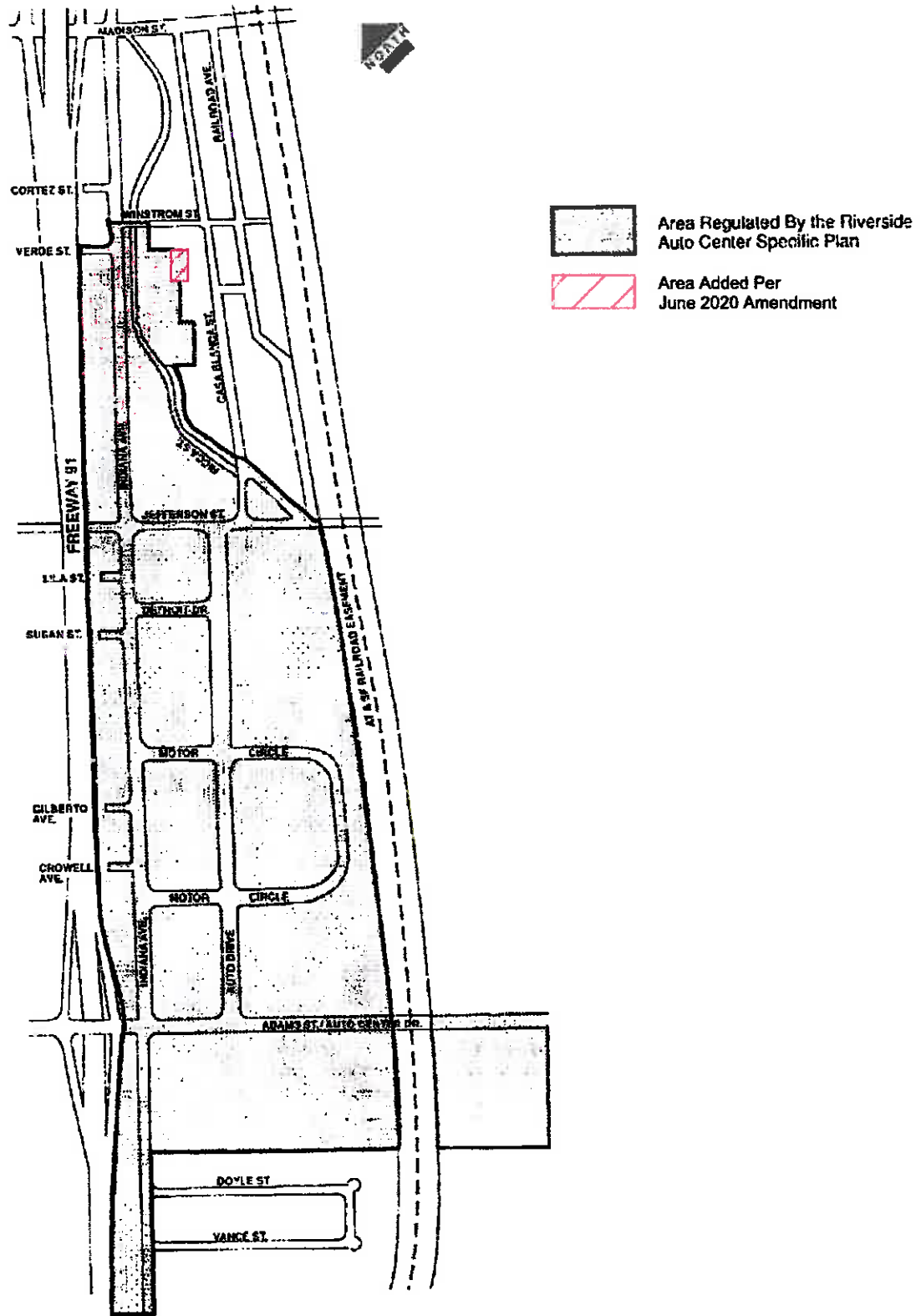


Figure 1 — Specific Plan Area



Amended June 2020

**NOTICE OF PUBLIC HEARING**  
**RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**  
[www.rcaluc.org](http://www.rcaluc.org)

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893**. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The City of Riverside may hold hearings on this item and should be contacted on non-ALUC issues. For more information please contact City Planner Ms. Judy Eguez at (951) 826-5800.

The proposed project application may be viewed at [www.rcaluc.org](http://www.rcaluc.org). Written comments may be submitted to the Riverside County ALUC by e-mail to [prull@rivco.org](mailto:prull@rivco.org) or by U.S. mail to Riverside County Administrative Center, 4080 Lemon Street, 14<sup>th</sup> Floor, Riverside, California 92501. Individuals with disabilities requiring reasonable modifications or accommodations, please telephone Barbara Santos at (951) 955-5132.

**PLACE OF HEARING:**                    **Riverside County Administration Center**  
                                                  **4080 Lemon Street, 1<sup>st</sup> Floor Board Chambers**  
                                                  **Riverside California**

**DATE OF HEARING:**                    **August 13, 2020**

**TIME OF HEARING:**                    **9:30 A.M.**

**Pursuant to Executive Order N-25-20, this meeting will be conducted by teleconference only. Information on how to participate in the hearing will be available on the ALUC website at [www.rcaluc.org](http://www.rcaluc.org)**

**CASE DESCRIPTION:**

ZAP1099RI20 – The Motorcycle Company (Representative: Adkan Engineers) – Related Case: City of Riverside Case Nos. P20-0214 (General Plan Amendment), P20-0215 (Specific Plan Amendment), P20-0216 (Rezone), P20-0217 (Design Review). The applicant proposes to expand the existing Harley Davidson facility for motorcycle storage increasing the total building area from 35,794 square feet to 48,194 square feet (an increase of 12,400 square feet), to be constructed on new parcels increasing the total site acreage from 2.2 acres to 2.47 acres (an increase of 0.27 acres), located southerly of Indiana Avenue, westerly of Winstrom Street, northerly of Casablanca Street, and easterly of Jefferson Street. The applicant also proposes to amend the newly added parcel's General Plan land use designation from Medium Density Residential to Commercial Regional Center, as well as a rezone from Residential R-1-7000 to Commercial General. The applicant also proposes to amend the Riverside Auto Center Specific Plan to include the new parcels into the specific plan boundaries (Airport Compatibility Zone E of the Riverside Municipal Airport Influence Area).



# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

## APPLICATION FOR MAJOR LAND USE ACTION REVIEW

Riv  
ALUC

ALUC CASE NUMBER: ZAP1099RI20      DATE SUBMITTED: June 10, 2020

### APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

Applicant	<u>The Motorcycle Company</u>	Phone Number	<u>(951) 785-0100</u>
Mailing Address	<u>7688 Indiana Ave. Riverside Ca, 92504</u>	Email	<u>GlenE@themotorcyclecompany.com</u>

Representative	<u>Adkan Engineers</u>	Phone Number	<u>(909) 688-0241</u>
Mailing Address	<u>6879 Airport Drive, Riverside, CA 92504</u>	Email	<u>mbrendecke@adkan.com</u>

Property Owner	<u>The Motorcycle Company</u>	Phone Number	<u>(951) 785-0100</u>
Mailing Address	<u>7688 Indiana Ave. Riverside Ca, 92504</u>	Email	<u>GlenE@themotorcyclecompany.com</u>

### LOCAL JURISDICTION AGENCY

Local Agency Name	<u>City Of Riverside</u>	Phone Number	<u>(951) 826-5800</u>
Staff Contact	<u>Judy Eguez</u>	Email	<u>jeguez@riversideca.gov</u>
Mailing Address	<u>3900 Main Street - 3rd Floor</u>	Case Type	<input checked="" type="checkbox"/> General Plan / Specific Plan Amendment <input checked="" type="checkbox"/> Zoning Ordinance Amendment <input type="checkbox"/> Subdivision Parcel Map / Tentative Tract <input type="checkbox"/> Use Permit <input checked="" type="checkbox"/> Site Plan Review/Plot Plan <input type="checkbox"/> Other
Local Agency Project No	<u>P 20-0214 (OPA); P 20-0215 (SPA); P 20-0216 (Rezone); P 20-0217 (Design Review)</u>		

### PROJECT LOCATION

*Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways*

Street Address	<u>7681 Casa Blanca St, Riverside CA 92504 - PORTION OF</u>		
	<u>7691 Casa Blanca St, Riverside CA 92504 - PORTION OF</u>		
Assessor's Parcel No.	<u>230-242-005, 230-242-006 - PORTIONS OF</u>	Gross Parcel Size	<u>11,985 SF</u>
Subdivision Name		Nearest Airport and distance from Airport	
Lot Number			

### PROJECT DESCRIPTION

*If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed*

Existing Land Use (describe)	<u>The 2 parcels fronting Casa Blanca have a current land use designation of Medium Density Residential.</u>
------------------------------	--------------------------------------------------------------------------------------------------------------

Proposed Land Use (describe)	The portion of the 2 parcels being accepted by The Motorcycle Company per a LLA proposes a land use designation of Commercial Regional Center.		
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)	_____	
For Other Land Uses (See Appendix C)	Hours of Operation	_____	
	Number of People on Site	Maximum Number	_____
	Method of Calculation	_____	
Height Data	Site Elevation (above mean sea level)	_____	858 ft.
	Height of buildings or structures (from the ground)	_____	25 ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?		<input type="checkbox"/> Yes
			<input checked="" type="checkbox"/> No
	If yes, describe	_____ _____ _____	

- A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:**
1. . . . . Completed ALUC Application Form
  1. . . . . ALUC fee payment
  1. . . . . Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
  1. . . . . Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
  1. . . . . CD with digital files of the plans (pdf)
  1. . . . . Vicinity Map (8.5x11)
  1. . . . . Detailed project description
  1. . . . . Local jurisdiction project transmittal
  3. . . . . Gummed address labels for applicant/representative/property owner/local jurisdiction planner
  3. . . . . Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. **(Only required if the project is scheduled for a public hearing Commission meeting)**

**SCHEDULE OF DEVELOPMENT REVIEW FEES (effective 3/1/19)**

CASE TYPE	ALL OTHERS		MARCH ZONE E	
	INITIAL REVIEW FEE	AMENDED REVIEW FEE	INITIAL REVIEW FEE	AMENDED REVIEW FEE
General Plan or General Plan Element (County or City)	\$3,696	\$2,458	\$2,310	\$1,537
Community Plan or Area Plan (County or City)	\$3,696	\$2,402	\$2,310	\$1,502
(New) Specific Plan or Master Plan	\$3,261	N/A	\$2,038	N/A
Specific Plan Amendment	N/A	\$2,181	N/A	\$1,363
General Plan Amendment	\$1,331	N/A	\$832	N/A
Change of Zone or Ordinance Amendment	\$1,331	\$887	\$832	\$554
Non-Impact Legislative Project (as determined by staff)	\$420	N/A	\$375	N/A
Tract Map	\$1,515	\$1,017	\$947	\$636
Conditional Use Permit or Public Use Permit	\$1,331	\$887	\$832	\$554
Plot Plan, Development Review Plan or Design Review	\$1,331	\$887	\$832	\$554
Parcel Map	\$1,331	\$887	\$832	\$554
Environmental Impact Report*	\$3,050	\$2,033	\$1,906	\$1,271
Other Environmental Assessments*	\$1,671	\$1,109	\$1,044	\$693
Building Permit or Tenant Improvement	\$573	\$389	\$359	\$243

Effective March 1, 2019, an additional fee of \$190.00 will be charged to projects requiring ALUC public hearings (no additional fee for staff review cases).

ADDITIONAL PROJECT SPECIFIC FEES (in addition to the above fees)				
Location in APZ I or II of March	\$2,500	\$2,500	N/A	N/A
AIA Large Commercial Solar Project (Energy Generation Facility)	\$3,000	\$3,000	\$3,000	\$3,000
Heliports/Helicopter Landing Sites	\$1,000	\$1,000	\$1,000	\$1,000
Speculative Nonresidential Multiple Buildings (4 or more)	\$8,210	\$8,210	N/A	N/A

NOTE: \* THIS FEE IS COLLECTED ONLY FOR PROJECTS THAT ARE NOT CLASSIFIED UNDER ONE OF THE ABOVE CATEGORIES.

**Checks should be made payable to: Riverside County Airport Land Use Commission**



**COUNTY OF RIVERSIDE  
AIRPORT LAND USE COMMISSION**

**STAFF REPORT**

**AGENDA ITEM:** 3.3

**HEARING DATE:** August 13, 2020

**CASE NUMBER:** ZAP1102FV20 – Platinum Collision (Representative: Jim Roachelle)

**APPROVING JURISDICTION:** County of Riverside

**JURISDICTION CASE NO:** CUP200008 (Conditional Use Permit)

**LAND USE PLAN:** 2007 French Valley Airport Land Use Compatibility Plan, as amended in 2011

Airport Influence Area: French Valley Airport

Land Use Policy: Airport Compatibility Zones B1 and C

Noise Levels: 55-60 CNEL from aircraft in westerly portion of site

**MAJOR ISSUES:** None

**RECOMMENDATION:** Staff recommends that the Commission find the Conditional Use Permit CONSISTENT, subject to the conditions included herein.

**PROJECT DESCRIPTION:** The applicant proposes to establish an auto collision repair facility within a 22,925 square foot commercial building (approved building B1) and construct a 560 square foot wash bay on a 1.62 acre parcel. Also proposed is a 122 parking stall vehicle storage yard on an adjoining 1.28 acre parcel (replacing approved 13,917 square foot commercial building B3).

The previous proposal (ZAP1079FV18) to construct fifteen commercial buildings with a combined gross floor area of 138,495 square feet on 14.19 acres was found consistent by the Commission on May 10, 2018.

**PROJECT LOCATION:** The site is located on the northerly side of Benton Road, easterly of its intersections with Winchester Road and Penfield Lane, and westerly of a northerly straight line extension of Leon Road, approximately 3,500 feet northeasterly of the northerly end of Runway 18-36 at French Valley Airport.

**BACKGROUND:**

Original ALUC Case ZAP1079FV18: The original project proposed fifteen commercial buildings with a combined gross floor area of 138,495 square feet on 14.19 acres. This included 42,340 square feet of retail area, 35,200 square feet of office area, 54,195 square feet of industrial/warehouse area, and 6,760 square feet of restaurant area. The project was found consistent with both the average and single acre intensity criteria.

Building B1 was located on a 1.62 acre parcel (0.19 acres in Zone B1 and 1.43 acres in Zone C), and proposed a 22,925 square foot building including 12,800 square feet of office area and 10,125 square feet of warehouse area. (The building was located entirely within Zone C, and no building was proposed in the Zone B1 portion of the parcel). The building accommodated 85 people, resulting in an average intensity of 59 people per acre, which was consistent with Zone C average intensity criterion of 80 people per acre. The single acre area included 4,000 square feet of office area and 12,800 square feet of warehouse area, resulting in a total occupancy of 72 people, which is consistent with Zone C single acre criterion of 160.

Building B3 was located on 1.28 acre parcel (0.92 acres in Zone B1 and 0.36 acres in Zone C), and proposed a 13,917 square foot building including 2,600 square feet of office area and 11,317 square feet of warehouse area. (The building was located entirely within Zone B1, and no building was proposed in the Zone C portion of the parcel). The building accommodated 36 people, resulting in an average intensity of 39 people per acre, which was consistent with Zone B1 average intensity criterion of 40 people per acre. The single acre area included 2,600 square feet of office area and 11,317 square feet of warehouse area, resulting in a total occupancy of 36 people, which is consistent with Zone B1 single acre criterion of 80.

Non-Residential Average Land Use Intensity: Pursuant to the French Valley Airport Land Use Compatibility Plan (ALUCP), the project is located within Compatibility Zones B1 and C. Zone B1 restricts average intensity to 40 people per acre, and Zone C restricts average intensity to 80 people per acre.

Current Revised Project ZAP1102FV20:

The proposed auto collision repair facility will utilize the approved 22,925 square foot commercial building (B1) located on a 1.62 acre parcel (0.19 acres in Zone B1 and 1.43 acres in Zone C) and propose a new 560 square foot wash bay. The project also includes replacing the approved 13,917 square foot commercial building B3 with a 122 parking stall vehicle storage yard on a 1.28 acre parcel (0.92 acres in Zone B1 and 0.36 acres in Zone C).

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan and the Additional Compatibility Policies included within the French Valley ALUCP, the following rates were used to calculate occupancy for the proposed building:

- Office – 1 person per 200 square feet (with 50% reduction),
- Lobby – 1 person per 15 square feet,
- Warehouse – 1 person per 500 square feet,

- Storage – 1 person per 300 square feet,
- Non-food retail (repairs) – 1 person per 170 square feet.

The project proposes to utilize the entire (approved) 22,925 square foot Building B1 (located entirely within Zone C) on a 1.62 acre parcel as an auto collision repair facility, including 16,406 square feet of repair area, 3,304 square feet of warehouse area, 1,521 square feet of office area, 200 square feet of lobby area, 100 square feet of storage area, and a 560 square foot wash bay located outside the building, accommodating a total occupancy of 129 people, resulting in an average intensity of 80 people per acre, which is consistent with the Zone C criterion of 80 people per acre. (There are no buildings proposed within the Zone B1 portion of this parcel).

The project also seeks to remove and replace the 13,917 square foot commercial Building B3 (located entirely within Zone B1) on a 1.28 acre parcel with a 122 parking stall vehicle storage yard to be used by the repair facility. No buildings are proposed on this parcel, therefore no occupancy will be generated using the Building Code Method. (There are no buildings proposed within the Zone C portion of this parcel).

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per standard vehicle). The project proposes 69 parking spaces for Building B1 parcel, which would accommodate an occupancy of 104 people, resulting in an average intensity of 64 people per acre, which is consistent with Compatibility Zone C average intensity criterion of 80 people per acre.

The project also proposes 122 parking spaces for Building B3 parcel, which would potentially accommodate an occupancy of 183 people using the parking code method, resulting in an average intensity of 143 people per acre, which is inconsistent with Compatibility Zone B1 average intensity criterion of 40 people per acre. However, this scenario is unrealistic as the proposed 122 “parking” spaces are not considered regular parking spaces that are open to the general public for use. The 122 parking spaces are located within a screened-in fenced area in association with the auto collision repair facility and will be used as vehicle long-term storage awaiting repairs. In addition, it is unlikely that people utilizing the auto repair facility is travelling with the average vehicle occupancy assumption of 1.5 people. Therefore, the use of the parking code method to determine occupancy for this site is not applicable, and that the parking code calculation used for the Building B1 is a truer representation of the parking code calculation for the project.

Non-Residential Single-Acre Land Use Intensity: Pursuant to the French Valley ALUCP, single-acre intensities are limited to 80 persons in any given acre area within Zone B1 and 160 person within Zone C.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre intensity is located over the office area of the building, including 10,588 square feet of repair area, 2,597 square feet of warehouse area, 1,521 square feet of office area, 200 square feet of lobby area, and 100 square feet of storage area, accommodating a single-acre occupancy of 89 people, which is

consistent with the Compatibility Zone C criterion of 160.

**Prohibited and Discouraged Uses:** The applicant does not propose any uses specifically prohibited or discouraged in Compatibility Zones B1 and C.

**Noise:** The French Valley ALUCP depicts the site as being in an area within the 55-60 CNEL range from aircraft noise. As a primarily industrial and commercial use not sensitive to noise (and considering typical anticipated building construction noise attenuation of approximately 20 dBA), the industrial and commercial areas would not require special measures to mitigate aircraft-generated noise. However, a condition is included to provide for adequate noise attenuation within office areas of the proposed buildings.

**Part 77:** The elevation of Runway 18-36 at its northerly terminus is 1,347 feet above mean sea level (1,347 feet AMSL). At a distance of approximately 3,500 feet from the runway to the closest parcel within the site, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,382 feet AMSL. The finished floor elevation for the original site ranges from 1,348 feet to 1,358 feet AMSL. The applicant team has accepted top point elevations not to exceed 1,382 feet above mean sea level, and acknowledges that, should that number be exceeded, a "Determination of No Hazard to Air Navigation" letter will be required for any building whose top point elevation exceeds the runway elevation by more than one foot for every 100 feet of distance from the northerly end of the runway at French Valley Airport.

The proposed auto collision facility would not increase building/structure heights beyond a top point elevation of 1,382 feet above mean sea level, and if it does, then a Determination of No Hazard to Air Navigation letter will be required for those buildings/structures. (A new 10 foot tall car wash bay is proposed, however, it is significantly shorter than approved building height and does not exceed the top point elevation of 1,382 feet AMSL).

**Open Area:** Compatibility Zone B1 requires 30%, Compatibility Zone C requires 20% and Compatibility Zone D requires 10% of the land area within major projects (10 acres or larger) be set aside as open area that could potentially serve as emergency landing areas, which was applied to the original project translating to 3.1 acres of required open area, which the applicant provided 3.1 acres of open area within the driveways and drive aisles of the site.

The proposed auto collision facility would not impact the ALUC open area.

**Hazards to Flight:** Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be

eliminated. (FAA Advisory Circular 5200-33C). The project is located 3,500 feet from the runway, and therefore would be subject to the above requirement.

The project proposes approximately 1,100 square feet of infiltration basin area located northwesterly of the 122 parking spaces for Building B3 parcel. Infiltration basins are prohibited in Zones B and C due to the potential that such areas could provide food, water, and shelter for hazardous wildlife. Pursuant to the study “Wildlife Hazard Management at Riverside County Airports: Background and Policy”, October 2018, by Mead & Hunt, which is the basis of the brochure titled “Airports, Wildlife and Stormwater Management”, such basins are potentially suitable within 10,000 feet of the airport only with appropriate modifications such as: 48 hours drawdown, steep slopes (steeper than 3:1), avoid landscaping or provide appropriate landscaping reviewed by a qualified biologist that will not be attractive to hazardous wildlife and adequately maintained, and consider the use of covers.

The applicant has agreed to a condition requiring that the basin complies with all parameters identified in the wildlife hazard policy for infiltration basins. Therefore, the project would be consistent with the goals and policies of the ALUC wildlife hazard study and FAA circular.

#### **CONDITIONS:**

1. Any outdoor lighting installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky and shall comply with the requirements of Riverside County Ordinance No. 655, as applicable.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)

- (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, places of worship, critical community infrastructure facilities, aboveground bulk storage of more than 6,000 gallons of hazardous or flammable materials, highly noise sensitive outdoor nonresidential uses, and hazards to flight.
3. The attached notice shall be provided to all prospective purchasers of the property and tenants or lessees of the buildings.
  4. Prior to recordation of a final map or issuance of building permits, whichever comes first, the landowner shall convey an avigation easement to the County of Riverside as owner of French Valley Airport. Contact the Riverside County Economic Development Agency – Aviation Division at (951) 955-9722 for additional information.
  5. The proposed infiltration basin shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the detention basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the infiltration basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at [RCALUC.ORG](http://RCALUC.ORG) which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist. The infiltration basin shall be designed in accordance with all parameters identified in the Wildlife Hazard Management at Riverside County Airports: Background and Policy, including 48 hours drawdown, steep slopes (steeper than 3:1), avoid landscaping or provide appropriate landscaping reviewed by a qualified biologist that will not be attractive to hazardous wildlife and adequately maintained, and consider the use of covers.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

6. Buildings shall be limited to a maximum top point elevation of 1,382 feet above mean sea level unless a “Determination of No Hazard to Air Navigation” letter authorizing a higher top point elevation has been issued by the Federal Aviation Administration Obstruction Evaluation Service for any building whose top point elevation exceeds the runway elevation by more than one foot for every 100 feet of distance from the northerly end of the runway at French Valley Airport.
7. Noise attenuation measures shall be incorporated into the design of the office areas of the proposed buildings, to the extent such measures are necessary to ensure that interior noise levels from aircraft operations are at or below 45 CNEL.
8. This project has been evaluated as 16,406 square feet of repair area, 3,304 square feet of warehouse area, 1,521 square feet of office area, 200 square feet of lobby area, 100 square feet of storage area, and a 560 square foot wash bay. Any increase in building area, change in use to any use other than offices, repair or warehousing, or modification of the lot lines and areas will require an amended ALUC review to evaluate consistency with the ALUCP compatibility criteria.
9. A minimum of 3.1 acres of ALUC-qualifying open area shall be provided on the project site, as shown on the exhibit entitled “ALUC Exhibit – Open Space.” No trees, poles, fencing, or trash enclosures, or any other permanent items greater than four feet in height and four inches in diameter shall be placed within these areas.
10. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission and Riverside County – Aviation Division as owner and operator of French Valley Airport. In the event of any reasonable complaint about glare related to aircraft operations, the applicant shall agree to such specific mitigation measures as determined or requested by Riverside County Aviation Division.

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



# NOTICE

**THERE IS AN AIRPORT NEARBY.  
THIS STORM WATER BASIN IS DESIGNED TO HOLD  
STORM WATER FOR ONLY 48 HOURS AND  
NOT TO ATTRACT BIRDS**

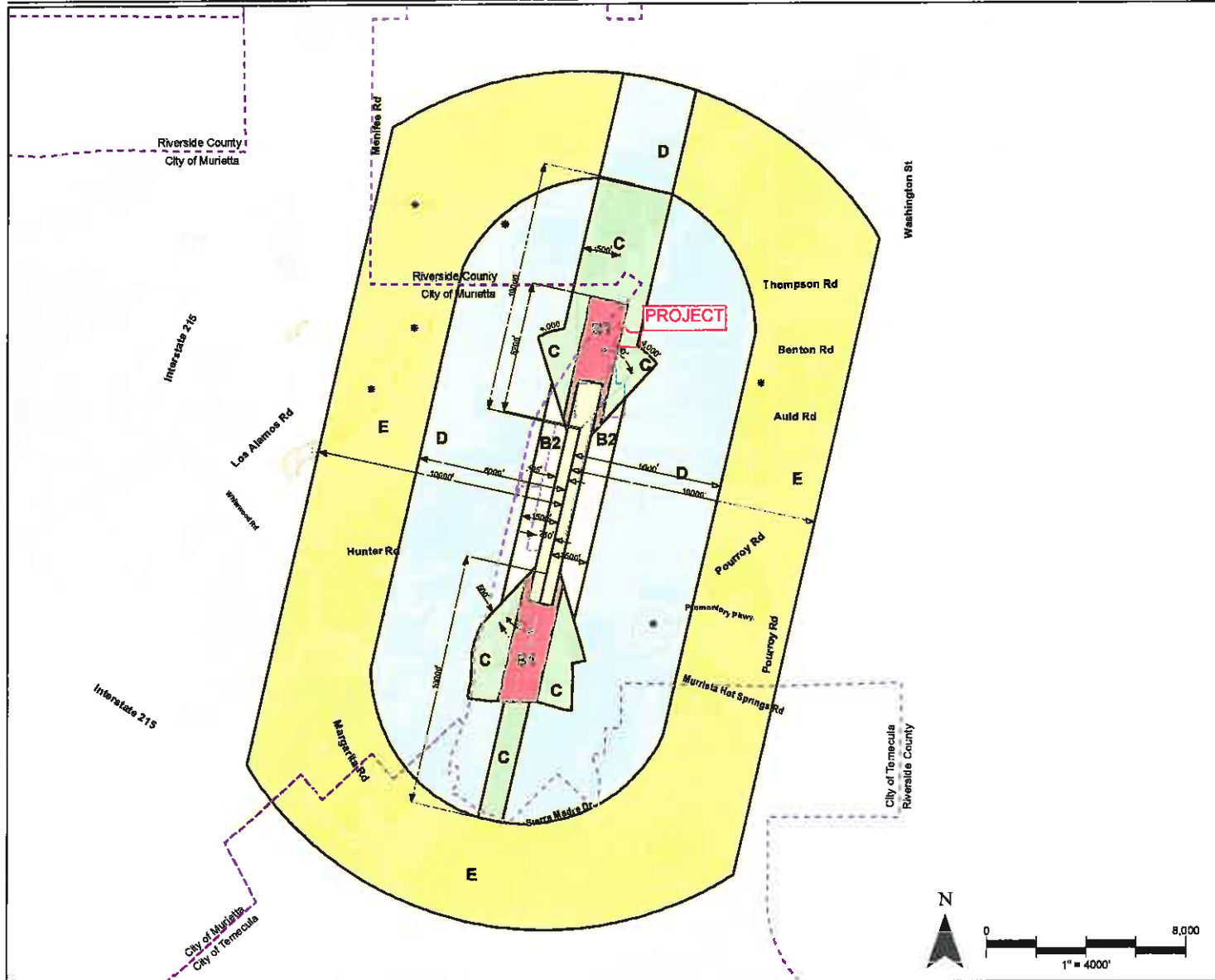
**PROPER MAINTENANCE IS NECESSARY TO AVOID  
BIRD STRIKES**



**IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:**

**Name:** \_\_\_\_\_

**Phone:** \_\_\_\_\_



**Legend**

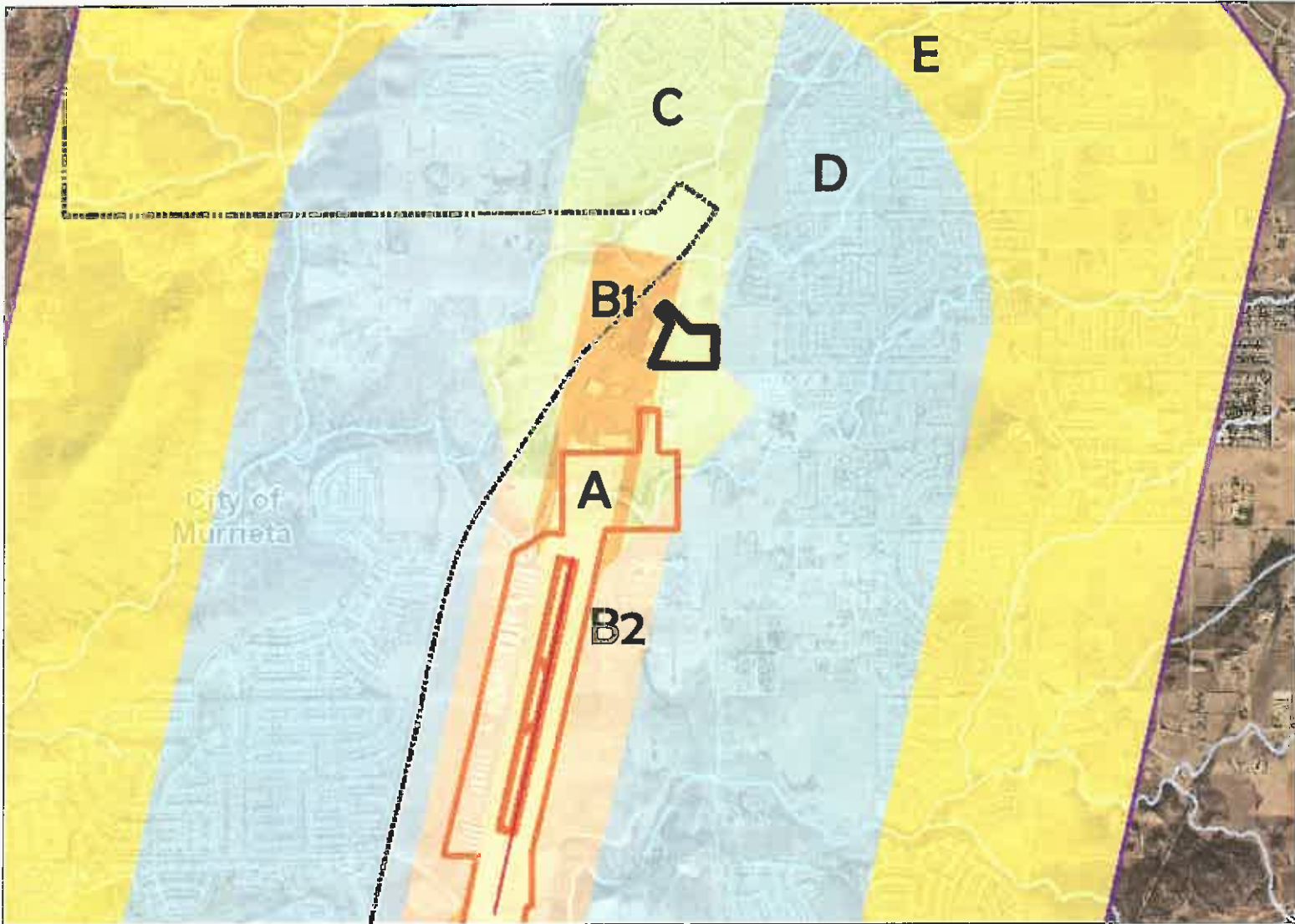
- Compatibility Zones**
- Airport Influence Area Boundary
  - Zone A
  - Zone B1
  - Zone B2
  - Zone C
  - Zone D
  - Zone E
- Boundary Lines**
- Airport Property Line
  - - - City Limits
  - \* Height Review Overlay Zone

**Note**  
 Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.  
 See Chapter 2, Table 2A from compatibility criteria associated with this map.

Riverside County  
 Airport Land Use Commission  
 Riverside County  
 Airport Land Use Compatibility Plan  
 Policy Document  
 (April 2010)

Map FV-1  
**Compatibility Map**  
 French Valley Airport

# Map My County Map



### Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6

**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

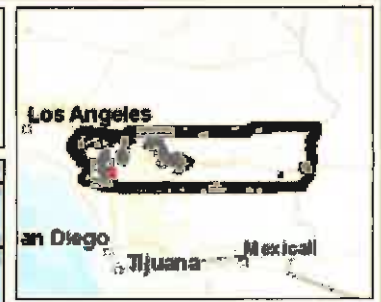
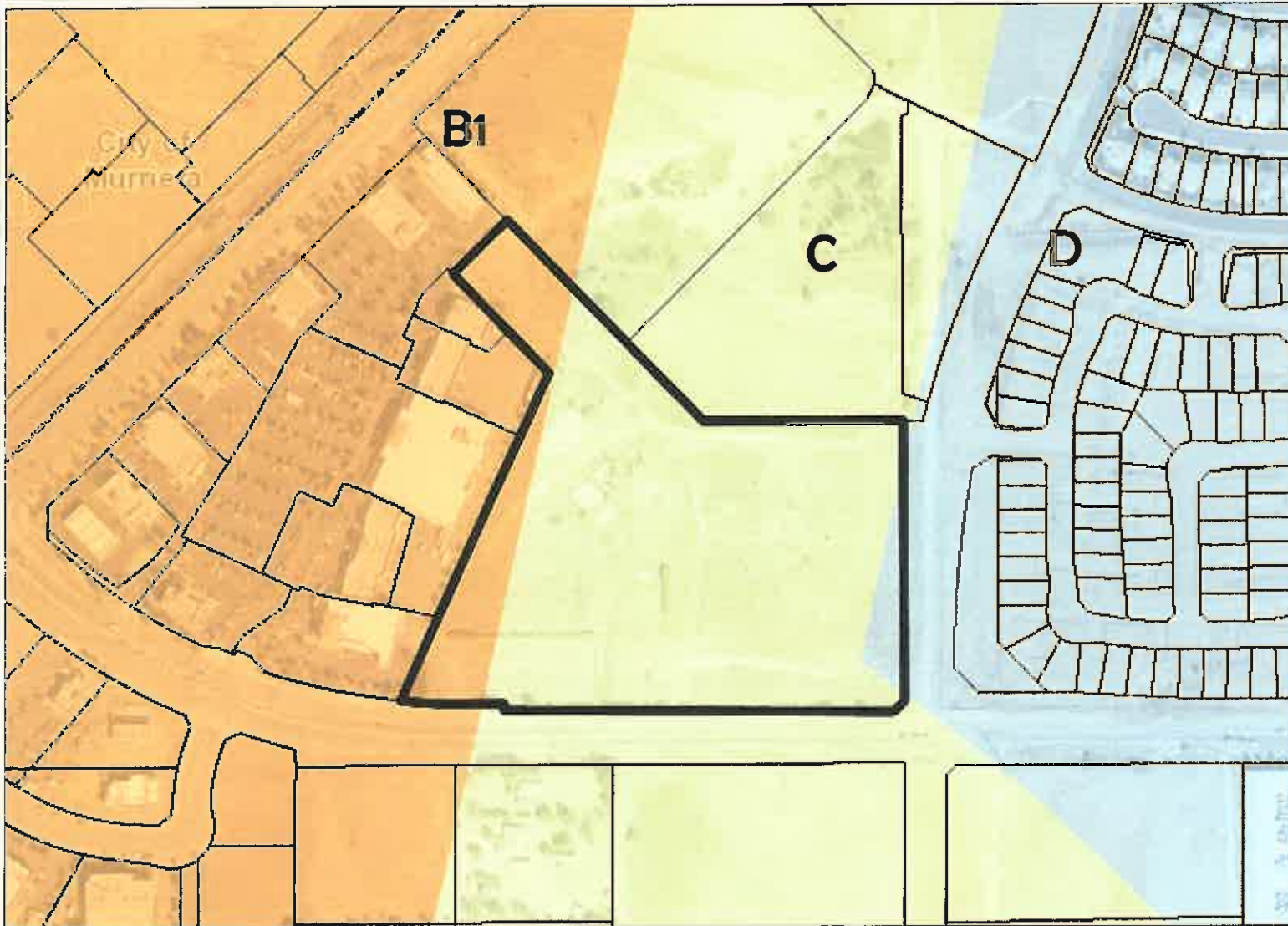


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### Notes

# Map My County Map



**Legend**

- Parcels
- Runways
- Airports
- Airport Influence Areas

**Airport Compatibility Zones**

- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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**Notes**

# Map My County Map



## Legend

-  Parcels
-  Blueline Streams
-  City Areas
-  World Street Map

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## Notes



0 385 770 Feet

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# Map My County Map



### Legend

- Blueline Streams
- City Areas
- World Street Map

### Notes

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# Map My County Map



## Legend

- Blue Line Streams
- City Areas
- World Street Map



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0 770 1,539 Feet

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## Notes

# Map My County Map



**Legend**

- Parcels
- Blueline Streams
- City Areas
- World Street Map



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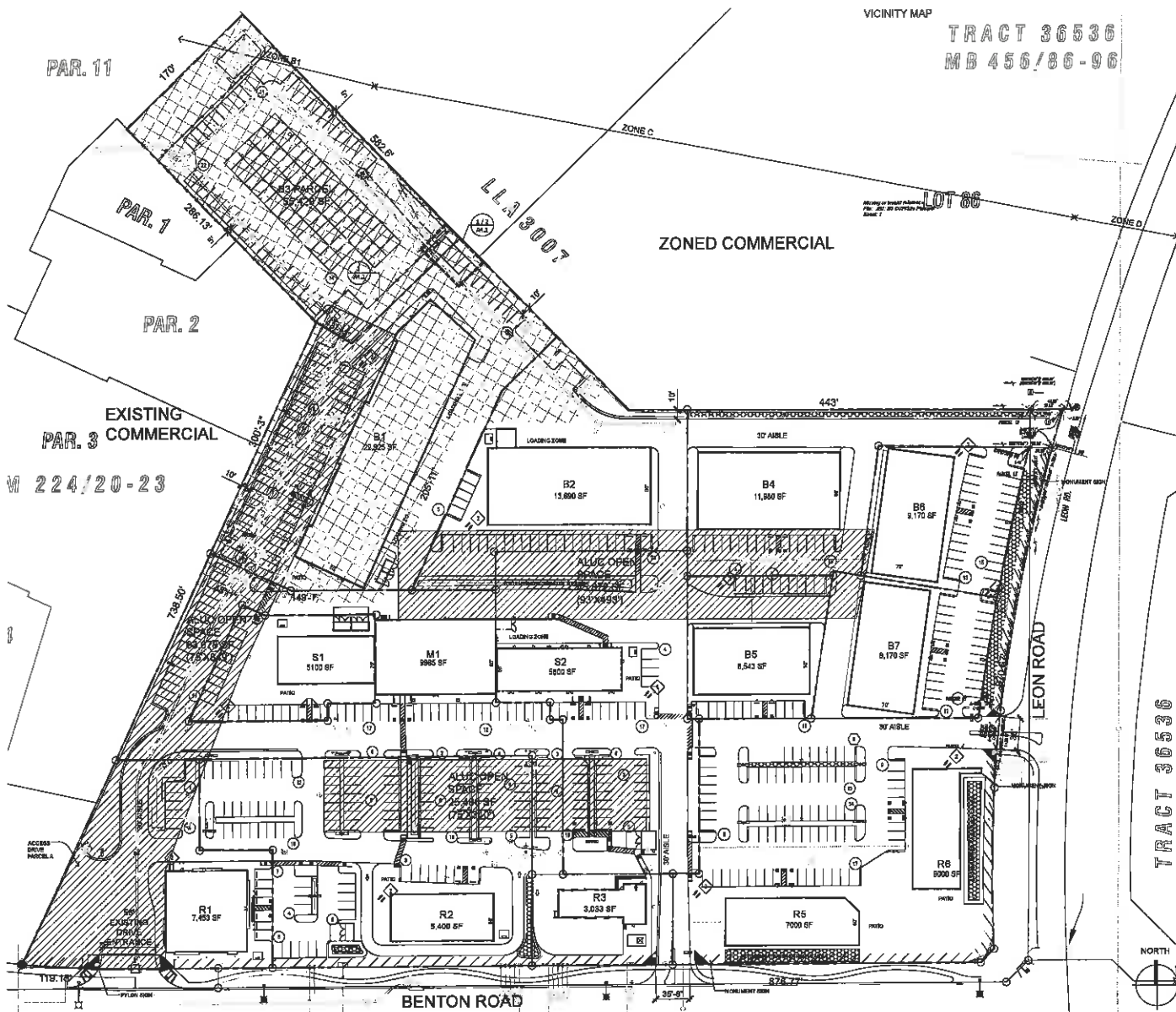


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**Notes**





VICINITY MAP  
**TRACT 36536**  
**MB 456/86-96**

**Project Data** **CUP 20008**

Project Address: Benton Road and Leon Road, French Valley, CA  
 APN: 963-060-032  
 PPT: 160016  
 FPM: 37399

Contact: Jim Roachelle (Owner Representative)  
 EJR Group  
 jim@ejrgroup.com  
 925-250-7118

Owner: Rob Mann  
 W Development Partners of Temecula, LLC

**Site Data**

RETAIL PHASE 1 206,681 SF (4.74 AC)  
 RETAIL PHASE 2 78,118 SF (1.80 AC)  
 BUSINESS PARK PHASE 1 185,684 SF (4.27 AC)  
 BUSINESS PARK PHASE 2 121,826 SF (2.80 AC)  
 PARCEL A ACCESS DRIVE 20,083 SF (0.45 AC)

**Totals: 612,405 SF (14.08 AC)**

Jurisdiction: County of Riverside  
 Zoning: Specific Plan (SP)  
 Land Use Designation: Commercial Retail (CP: CR)  
 Boundaries Information: THIS PLAN HAS BEEN PREPARED BY USING SITE DATA INFORMATION FROM SWS ENGINEERING INC.

Utility Company: SCE: Southern California Edison  
 Cable Company: Frontier  
 Water: Eastern Municipal Water District  
 Gas: Southern California Gas

**Building/ Parking Data**

RETAIL PHASE 1:						Parcel (SF)	ADA Stalls Total Stalls Required Provided
Building	Area (SF)	Required Parking	Provided Parking	Parking Ratio (per 1,000)			
M1	9,865	35	54	8.4	45,676	039	
B1	6,526	28	60	11.6	3,465	058	
B2	6,506	32	37	5.4	57,036	36	
R1	7,453	41	49	6.6	38,540		
R2	6,432	32	49	7.4	58,589		
B3	3,002	17	21	8.6	33,861		
<b>Totals</b>	<b>35,781</b>	<b>202</b>	<b>281</b>	<b>7.1</b>	<b>245,018</b>		

RETAIL PHASE 2:						Parcel (SF)	Bicycle Parking Required (19% of Water Spaces) Provided
Building	Area (SF)	Required Parking	Provided Parking	Parking Ratio (per 1,000)			
B5	7,000	31	41	5.9		3	
B6	6,000	33	41	6.8	76,118	24	
<b>Totals</b>	<b>13,000</b>	<b>72</b>	<b>82</b>	<b>8.3</b>		<b>27</b>	

BUSINESS PARK PHASE 1:						
Building	Area (SF)	Required Parking	Provided Parking	Parking Ratio (per 1,000)	Parcel (SF)	Max. Office
B1	29,925	11	69	3.5	75667	12,400
B2	19,690	11	33	3.5	13782	4,600
B3	0	0	122	0	99787	
<b>Totals</b>	<b>36,615</b>	<b>23</b>	<b>219</b>	<b>8.8</b>	<b>180,119</b>	<b>17,000</b>

BUSINESS PARK PHASE 2:						
Building	Area (SF)	Required Parking	Provided Parking	Parking Ratio (per 1,000)	Parcel (SF)	Max. Office
B4	1,580	1	23	11.4	36,421	3,800
B5	3,849	1	36	2.3	28,033	3,400
B6	8,170	1	7	0.8	21,321	4,800
B7	8,170	1	34	2.6	25,244	4,200
<b>Totals</b>	<b>28,243</b>	<b>4</b>	<b>74</b>	<b>3.4</b>	<b>121,529</b>	<b>16,000</b>

**FRENCH VALLEY COMMONS** FRENCH VALLEY, CALIFORNIA



W DEVELOPMENT PARTNERS OF TEMECULA, LLC

**DWG: 0F 38**  
 DATE: June 30, 2020  
 SCALE: 1"=50'

**A1.0**

VICINITY MAP

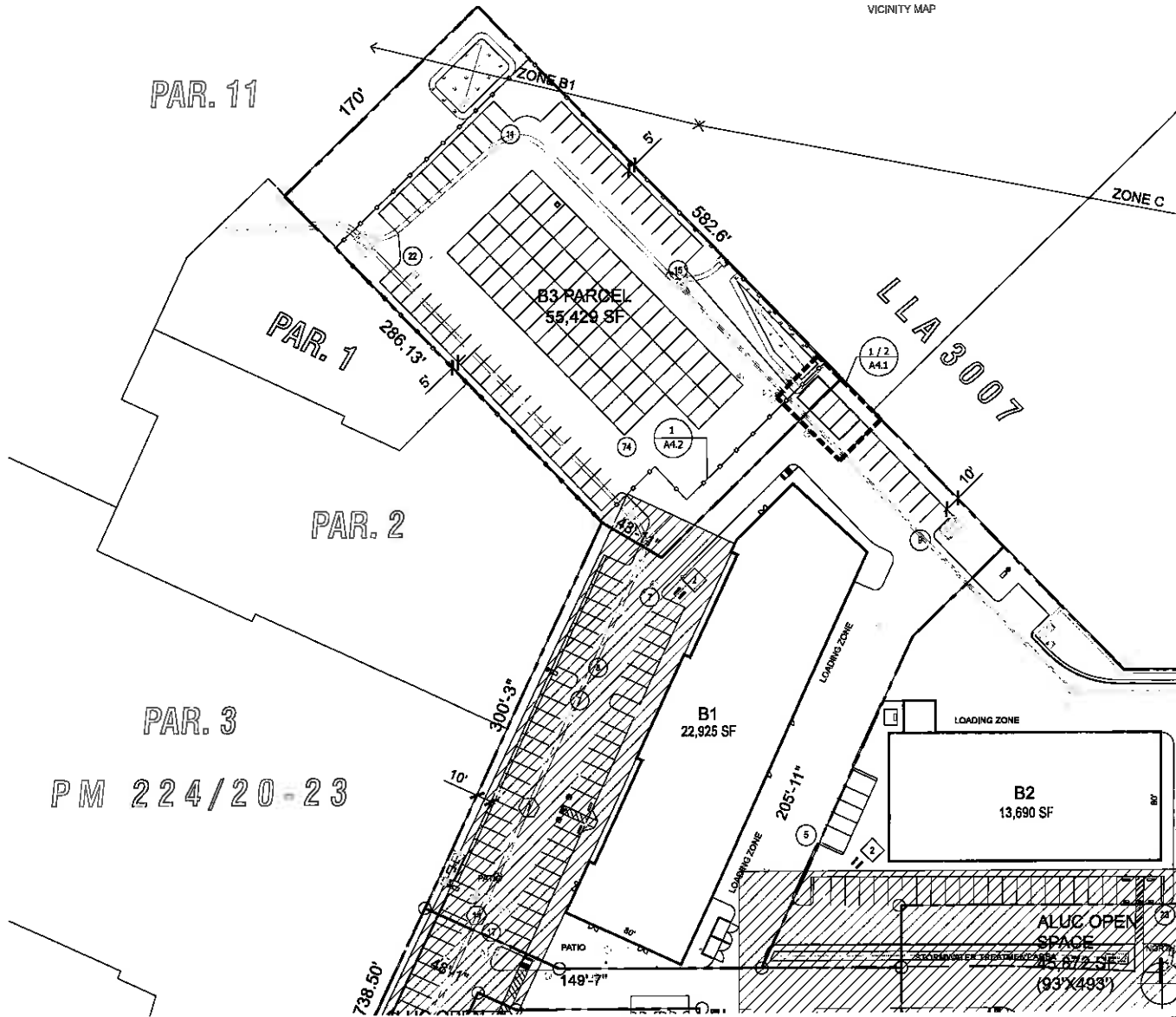
**Building/ Parking Data**

**CUP 20008**

BUSINESS PARK PHASE 1:

Building	Area (SF)	Required Parking	Provided Parking	Parking Ratio (per 1,000)	Parcel (SF)	Max. Office
B1	22,825	24	69	3.0	70567	2,780
B2	9	0	22	0	69787	2780
Totals	22,825	24	91	3.9	128,354	2780

COUNTY PARKING REQUIREMENTS:  
 WAREHOUSE: 1/2 Spaces/ 1,000 SF Floor Area  
 OFFICE: 5 (1,000 SF Floor Area)



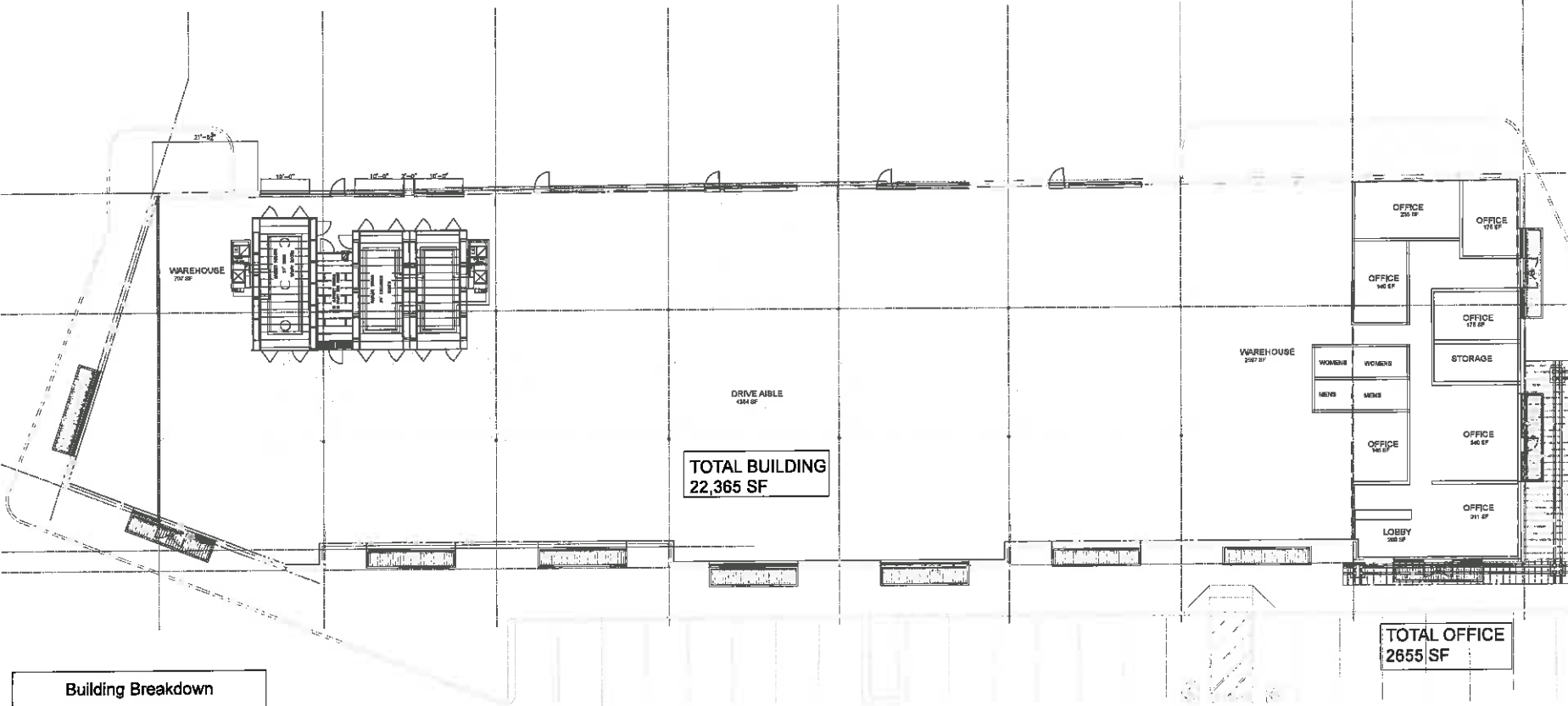
PM 224/20-23

**FRENCH VALLEY COMMONS** FRENCH VALLEY, CALIFORNIA

**JR** W DEVELOPMENT PARTNERS OF TEMECULA, LLC

**DWG:** OF 36  
 DATE: June 30, 2020  
 SCALE: 1"=50'

**A1.1**

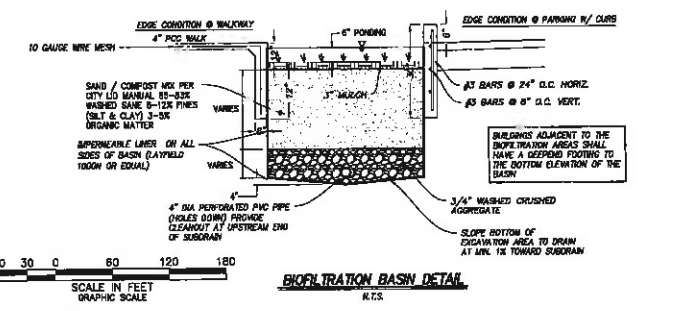
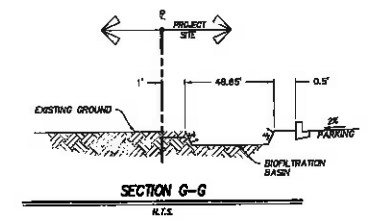
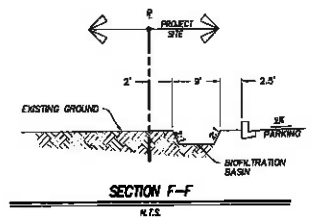
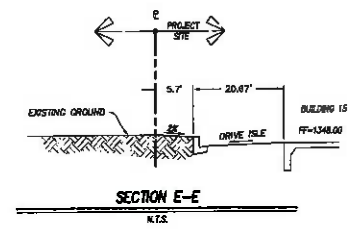
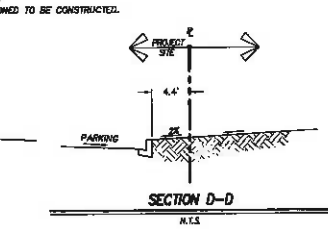
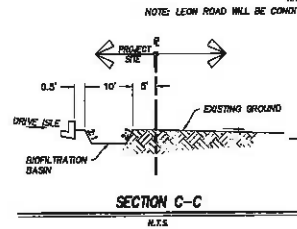
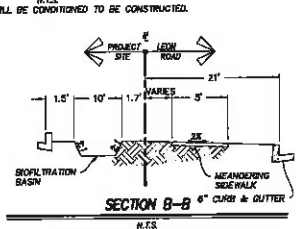
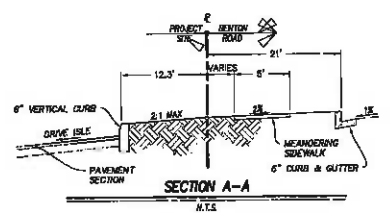
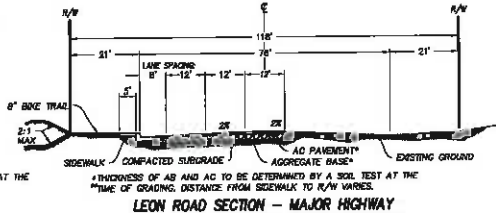
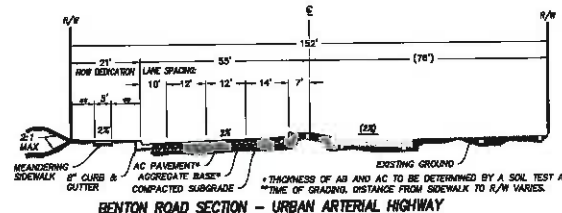
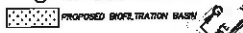


Building Breakdown	
	SF
Office	1,411
Lobby	200
Drive Aisle	4,364
Service	12,042
Warehouse	3,304



**CONSTRUCTION NOTES**

- 1 2 1/2" PRE-CAST CONCRETE INLET
- 2 CONCRETE NEARWALL
- 3 STORM DRAIN MANHOLE PER RFGC 10281
- 4 DATCH BASIN #6 PER RFGC 01182
- 5 3/4" PRE-CAST CONCRETE INLET
- 6 CONNECT TO EXISTING 36" ROP
- 7 4"X4" PRE-CAST CONCRETE INLET



SCALE IN FEET GRAPHIC SCALE

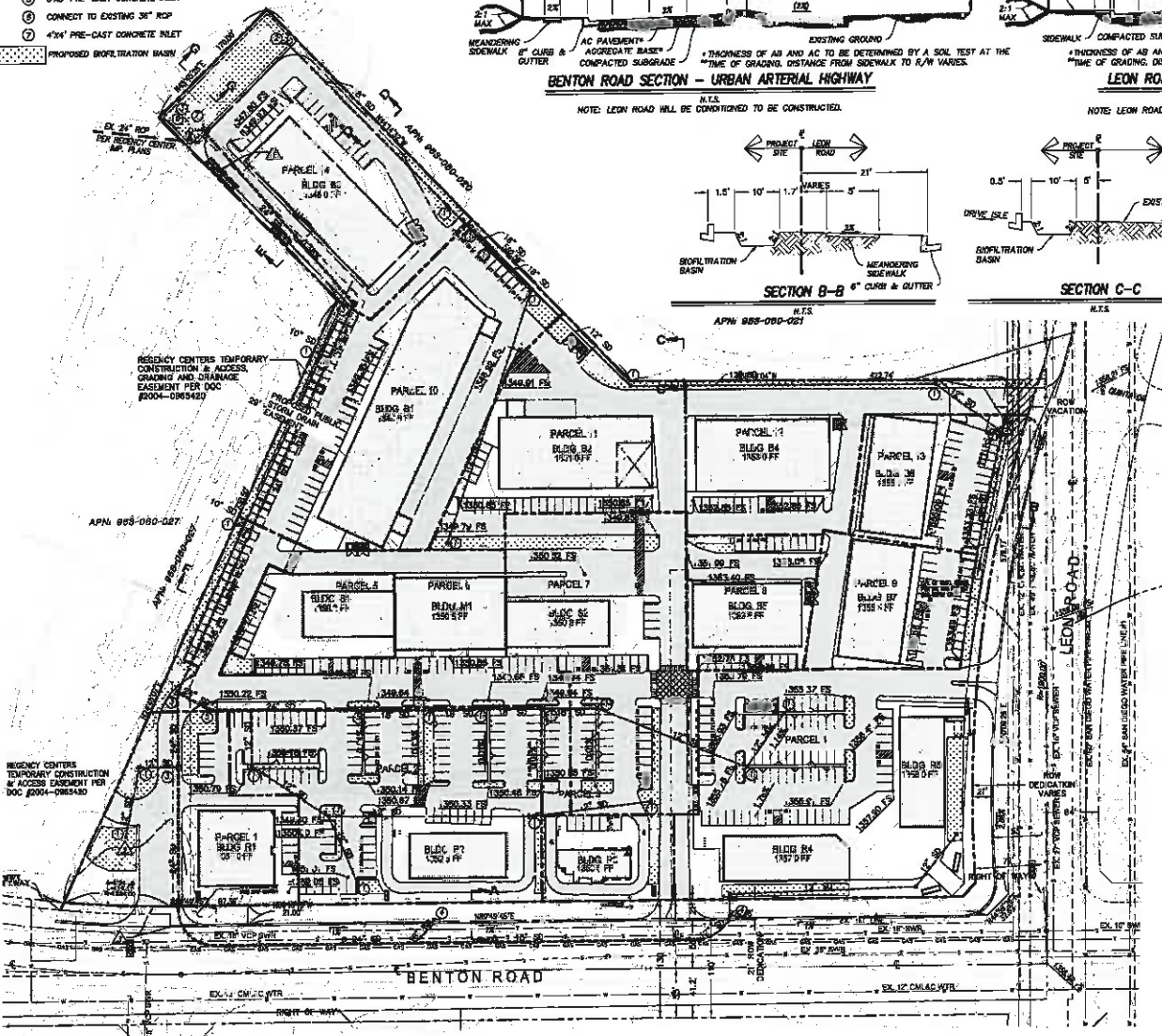
- EXISTING GRADEMENTS**
- REGENCY CENTERS TEMPORARY CONSTRUCTION & ACCESS, GRADING & DRAINAGE EASEMENT PER DOC #2004-085420
  - REGENCY CENTERS TEMPORARY CONSTRUCTION & ACCESS, GRADING & DRAINAGE EASEMENT PER DOC # 2004-085420
  - R/W DEDICATION PER DOC. # 2007-0758764

- NOTES**
1. ORNWAYS SHALL BE CONSTRUCTED TO COUNTY STANDARD NO. 2071A.
  2. THERE ARE CURRENTLY NO STREET LIGHTS ON BENTON ROAD ADJACENT TO THE PROJECT SITE.

**PRELIMINARY EARTHWORK**

CUT	11,738 CY
FILL	19,758 CY
IMPORT	8,060 CY

**SWS ENGINEERING, INC.**  
 CIVIL ENGINEERS • LAND PLANNERS • ENVIRONMENTAL ENGINEERS  
 311 South Birch, Suite 115 • 11401 Truxtun Avenue, Suite 311  
 Las Vegas, NV 89109 • Tempe, AZ 85285  
 P: 702-341-8811 • F: 702-744-0004 • E: info@sweinc.com



**FRENCH VALLEY COMMONS** FRENCH VALLEY, CALIFORNIA

PRELIMINARY GRADING PLAN

W DEVELOPMENT PARTNERS OF TEMECULA, LLC

DWG: 04 OF 38  
 DATE: May 02, 2018  
 SCALE: NTS

**C1.0**

IN THE UNINCORPORATED TERRITORY OF THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA  
**PARCEL MAP NO. 37399**

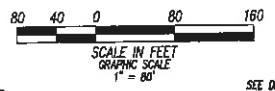
BEING A SUBDIVISION OF PARCEL B IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS SHOWN ON LOT LINE ADJUSTMENT NO. 4444 RECORDED MARCH 14, 2002 AS DOCUMENT NO. 2002-130502 OF OFFICIAL RECORDS, AND PERFECTED IN DEED RECORDED JANUARY 1, 2008 AS DOCUMENT NO. 2008-0068567 OF OFFICIAL RECORDS, ALL IN THE OFFICE OF THE COUNTY RECORDER OF RIVERSIDE COUNTY, CALIFORNIA ALSO BEING A PORTION OF SECTION 6, TOWNSHIP 7 SOUTH, RANGE 2 WEST, SAN BERNARDINO MERIDIAN.  
 SWS ENGINEERS, INC.  
 DATE OF SURVEY: MAY, 2008

LINE TABLE

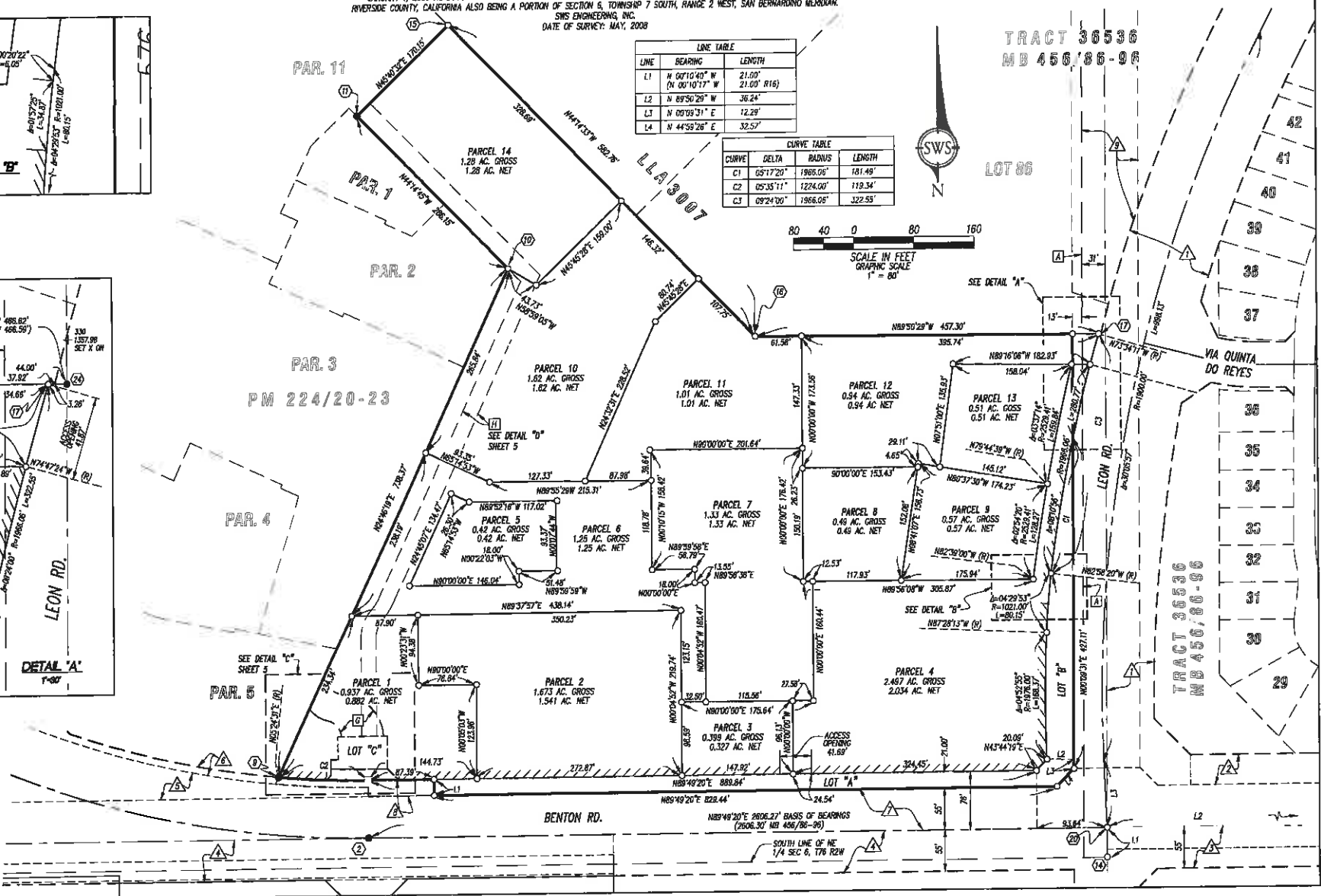
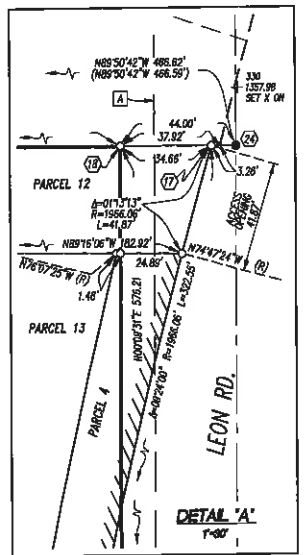
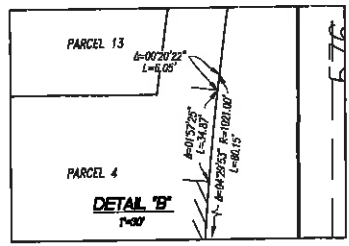
LINE	BEARING	LENGTH
L1	N 00°10'40" W (N 00°10'17" W)	21.00' 21.00' (R16)
L2	N 89°50'29" W	36.24'
L3	N 09°09'31" E	12.29'
L4	N 44°59'28" E	32.57'

CURVE TABLE

CURVE	DELTA	RADIUS	LENGTH
C1	05°17'29"	1965.05'	181.49'
C2	05°35'11"	1224.00'	119.34'
C3	09°24'00"	1966.05'	322.53'



TRACT 36536  
 MB 456/86-96



TRACT 36536  
 MB 456/86-96



**NOTICE OF PUBLIC HEARING**  
**RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**  
[www.rcaluc.org](http://www.rcaluc.org)

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893**. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The County of Riverside Planning Department should be contacted on non-ALUC issues. For more information please contact County of Riverside Planner Ms. Deborah Bradford at (951) 955-6646.

The proposed project application may be viewed at [www.rcaluc.org](http://www.rcaluc.org). Written comments may be submitted to the Riverside County ALUC by e-mail to [prull@rivco.org](mailto:prull@rivco.org) or by U.S. mail to Riverside County Administrative Center, 4080 Lemon Street, 14<sup>th</sup> Floor, Riverside, California 92501. Individuals with disabilities requiring reasonable modifications or accommodations, please telephone Barbara Santos at (951) 955-5132.

**PLACE OF HEARING:** Riverside County Administration Center  
4080 Lemon Street, 1<sup>st</sup> Floor Board Chambers  
Riverside California

**DATE OF HEARING:** August 13, 2020

**TIME OF HEARING:** 9:30 A.M.

Pursuant to Executive Order N-25-20, this meeting will be conducted by teleconference only. Information on how to participate in the hearing will be available on the ALUC website at [www.rcaluc.org](http://www.rcaluc.org)

**CASE DESCRIPTION:**

ZAP1102FV20 – Platinum Collision (Representative: Jim Roachelle) – County of Riverside Case No. CUP200008 (Conditional Use Permit). A proposal to establish an auto collision repair facility within a 22,925 square foot commercial building (approved building B1) and construct a 560 square foot wash bay on a 1.62 acre parcel. Also proposed is a 122 parking stall vehicle storage yard on an adjoining 1.28 acre parcel (replacing approved 13,917 square foot commercial building B3) located northerly of Benton Road, easterly of its intersections with Winchester Road and Penfield Lane, and westerly of a northerly straight line extension of Leon Road (The previous proposal to construct fifteen commercial buildings with a combined gross floor area of 138,495 square feet at this site had been found consistent by the ALUC) (Airport Compatibility Zones B1 and C of the French Valley Airport Influence Area).



# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

## APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP1102FU20 DATE SUBMITTED: 7-1-20

### APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

Applicant TOM GREGG Phone Number 951-833-0493  
 Mailing Address PLATINUM COLLISION Email tom@platinumocs.com  
12483 BELLEGRAVE AVE  
EASTVALE, CA 91752

Representative JIM ROACHELLE Phone Number 925-250-7118  
 Mailing Address 344 20TH STREET Email jim@ejrgroup.com  
OAKLAND, CA 94612

Property Owner BOB MANN Phone Number 415-990-3378  
 Mailing Address 125 STILLMAN ST. Email robmann@fortunecap.com  
SAN FRANCISCO, CA 94107

F.V.  
B1

### LOCAL JURISDICTION AGENCY

Local Agency Name RIVERSIDE COUNTY Phone Number 951-955-6646  
 Staff Contact DEBORAH BRADFORD Email dbradfor@rivco.org  
 Mailing Address 4080 LEMON ST, 12TH FLOOR Case Type  
RIVERSIDE, CA 92501  
 General Plan / Specific Plan Amendment  
 Zoning Ordinance Amendment  
 Subdivision Parcel Map / Tentative Tract  
 Local Agency Project No CUP 200008 & CEQ 200020  Use Permit  
 Site Plan Review/Plot Plan  
 Other

### PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address 30880 BENTON ROAD  
WINCHESTER, CA  
 Assessor's Parcel No. 9163-060-032 Gross Parcel Size 131,892  
 Subdivision Name FRENCH VALLEY COMMONS Nearest Airport and distance from Airport FRENCH VALLEY AIRPORT  
 Lot Number 11 & 15

### PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe) VACANT LAND - CURRENTLY UNDER CONSTRUCTION



Proposed Land Use (describe)	B1 PARCEL: 22,925 SF INDUSTRIAL BUILDING AUTO B3 PARCEL: 55,492 SF PARCEL TO BE USED AS PARKING FOR B1 PROPOSED USE REDUCES TOTAL OCCUPANCY FROM 188 TO 113	
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)	
For Other Land Uses (See Appendix C)	Hours of Operation	7-6P M-F, 8A-1PM SATURDAY
	Number of People on Site	Maximum Number 113
	Method of Calculation	1 PERSON PER 200 SF
Height Data	Site Elevation (above mean sea level)	1377.50' (NO CHANGES FROM ORIGINAL ft. APPROVAL)
	Height of buildings or structures (from the ground)	28' (NO CHANGES FROM ORIGINAL APPROVAL) ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
	if yes, describe	

- A. **NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. **REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. **SUBMISSION PACKAGE:**
1. . . . . Completed ALUC Application Form
  1. . . . . ALUC fee payment
  1. . . . . Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
  1. . . . . Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
  1. . . . . CD with digital files of the plans (pdf)
  1. . . . . Vicinity Map (8.5x11)
  1. . . . . Detailed project description
  1. . . . . Local jurisdiction project transmittal
  3. . . . . Gummed address labels for applicant/representative/property owner/local jurisdiction planner
  3. . . . . Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. (Only required if the project is scheduled for a public hearing Commission meeting)

✓

**COUNTY OF RIVERSIDE  
AIRPORT LAND USE COMMISSION**

**STAFF REPORT**

**AGENDA ITEM:** 3.4

**HEARING DATE:** August 13, 2020

**CASE NUMBER:** ZAP1081BD20 – F2 Palm Desert LLC (Representative: Alberto Marciano)

**APPROVING JURISDICTION:** County of Riverside

**JURISDICTION CASE NO:** CUP190031 (Conditional Use Permit)

**LAND USE PLAN:** 2004 Bermuda Dunes Airport Land Use Compatibility Plan

**Airport Influence Area:** Bermuda Dunes Airport

**Land Use Policy:** Compatibility Zone C

**Noise Levels:** 55 - 60 CNEL contour

**MAJOR ISSUES:** None

**RECOMMENDATION:** Staff recommends that the Conditional Use Permit be found CONSISTENT, subject to the conditions included herein.

**PROJECT DESCRIPTION:** The applicant proposes to establish a cannabis retail dispensary facility within an existing 2,572 square foot building on 0.94 acres.

**PROJECT LOCATION:** The site is located easterly of Berkey Drive, westerly of Washington Street, and northerly of Varner Road, approximately 7,190 feet northwesterly of Runway 10-28 at Bermuda Dunes Airport.

**BACKGROUND:**

Non-Residential Average Intensity: Pursuant to the 2004 Bermuda Dunes Airport Land Use Compatibility Plan, the project site is located within Compatibility Zone C which restricts average intensity to 75 people per acre.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, the following rate could potentially be used to calculate the occupancy for the proposed building in Compatibility Zone C:

- Retail – 1 person per 60 square feet.
- Office – 1 person per 200 square feet
- Lobby, Reception, Break Room – 1 person per 15 square feet, and
- Storage – 1 person per 300 square feet.

The project proposes to establish a cannabis dispensary within the existing 2,572 square foot building, which includes 1,113 square feet of retail area, 334 square feet of office area, 422 square feet of assembly area, and 83 square feet of storage area, accommodating 50 people, resulting in an average intensity of 53 people per acre, which is consistent with the Compatibility Zone C criterion of 75.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle). Based on the number of parking spaces (30 spaces) provided, the total occupancy would be estimated at 45 people for an average intensity of 48 persons per acre, which is consistent with the Compatibility Zone C average criterion of 75.

Non-Residential Single-Acre Intensity: As previously noted, the proposed project is located within Compatibility Zone C which restricts non-residential intensity to 150 people in any given single acre.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre area would include 1,113 square feet of retail area, 334 square feet of office area, 422 square feet of assembly area, and 83 square feet of storage area, resulting in a single acre occupancy of 50 people, which is consistent with the Compatibility Zone C single acre criterion of 150.

Prohibited and Discouraged Uses: The applicant does not propose any uses specifically prohibited or discouraged in Compatibility Zone C of the Bermuda Dunes Airport Influence Area.

Noise: The site is located within the 55-60 CNEL contour range from aircraft noise. Office uses are identified as normally acceptable within this range; however, staff is recommending a condition to incorporate noise attenuation measures into the design of the proposed buildings to such extent as may be required to ensure that interior noise levels from aircraft operations are at or below 45 CNEL.

Part 77: The elevation of Runway 10-28 at its westerly terminus is approximately 73 feet above mean sea level (AMSL). At a distance of approximately 7,190 feet from the runway, FAA review would be required for any structures with top of roof exceeding 144 feet AMSL. The project's finished floor elevation is 105 feet AMSL, and the maximum height of the existing building is 20 feet, for a maximum top point elevation of 125 feet AMSL. The project does not propose to increase the height of the existing building. Therefore, Federal Aviation Administration (FAA) obstruction evaluation review for height/elevation reasons is not required.

Open Area: The site is located within Compatibility Zone C of the Bermuda Dunes Airport Influence Area, which requires projects 10 acres or larger to designate 20% of project area as ALUC qualifying open area that could potentially serve as emergency landing areas. Since the overall project size is less than 10 acres, the open area requirement is not applicable to this project.

**CONDITIONS:**

1. Any outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
  - (a) Any use or activity which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use or activity which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use or activity which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area.
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Highly noise-sensitive outdoor nonresidential uses.
3. The following uses/activities are specifically prohibited at this location: trash transfer stations that are open on one or more sides; recycling centers containing putrescible wastes; children's schools; daycare centers; libraries; hospitals; nursing homes.
4. The attached notice shall be given to all prospective purchasers and/or tenants of the property, and shall be recorded as a deed notice.
5. Any new detention basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the detention basin(s) shall not include

trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at [RCALUC.ORG](http://RCALUC.ORG) which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

6. Noise attenuation measures shall be incorporated into the design of the buildings, to the extent such measures are necessary to ensure that interior noise levels from aircraft operations are at or below 45 CNEL.
7. The project has been evaluated for 1,113 square feet of retail area, 334 square feet of office area, 422 square feet of assembly area, and 83 square feet of storage area. Any increase in building area or intensification of floor area usage will require review by the Airport Land Use Commission.

# **NOTICE OF AIRPORT IN VICINITY**

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

# NOTICE

**THERE IS AN AIRPORT NEARBY.  
THIS STORM WATER BASIN IS DESIGNED TO HOLD  
STORM WATER FOR ONLY 48 HOURS AND  
NOT TO ATTRACT BIRDS**

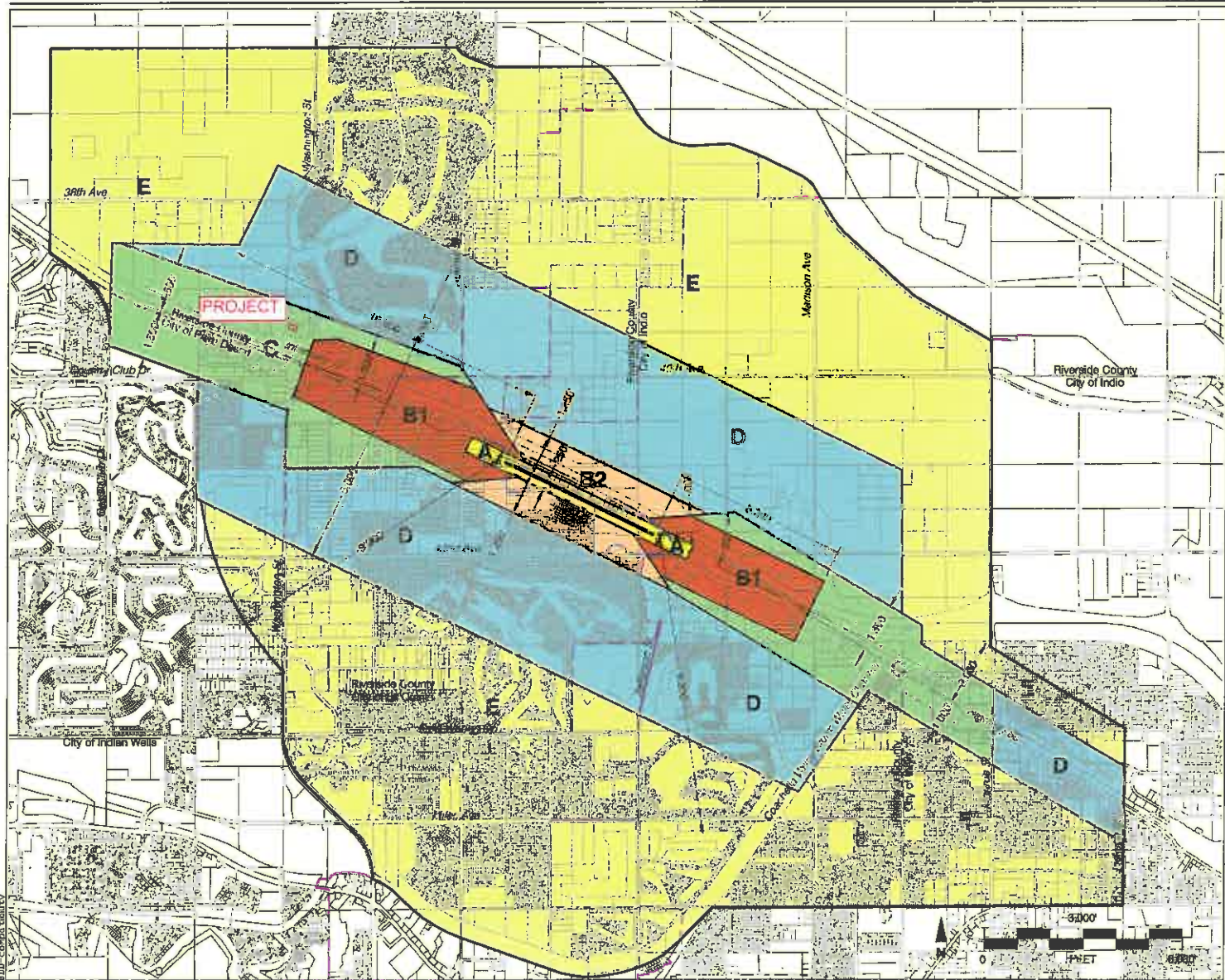
**PROPER MAINTENANCE IS NECESSARY TO AVOID  
BIRD STRIKES**



**IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:**

**Name:** \_\_\_\_\_

**Phone:** \_\_\_\_\_



**Legend**

**Compatibility Zones**

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E

**Boundary Lines**

- Airport Property Line
- City Limits

**Note**

Southwestern edge of Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

See Chapter 2, Table 2A for compatibility criteria associated with this map.

**Riverside County  
Airport Land Use Commission  
Riverside County  
Airport Land Use Compatibility Plan  
Policy Document**

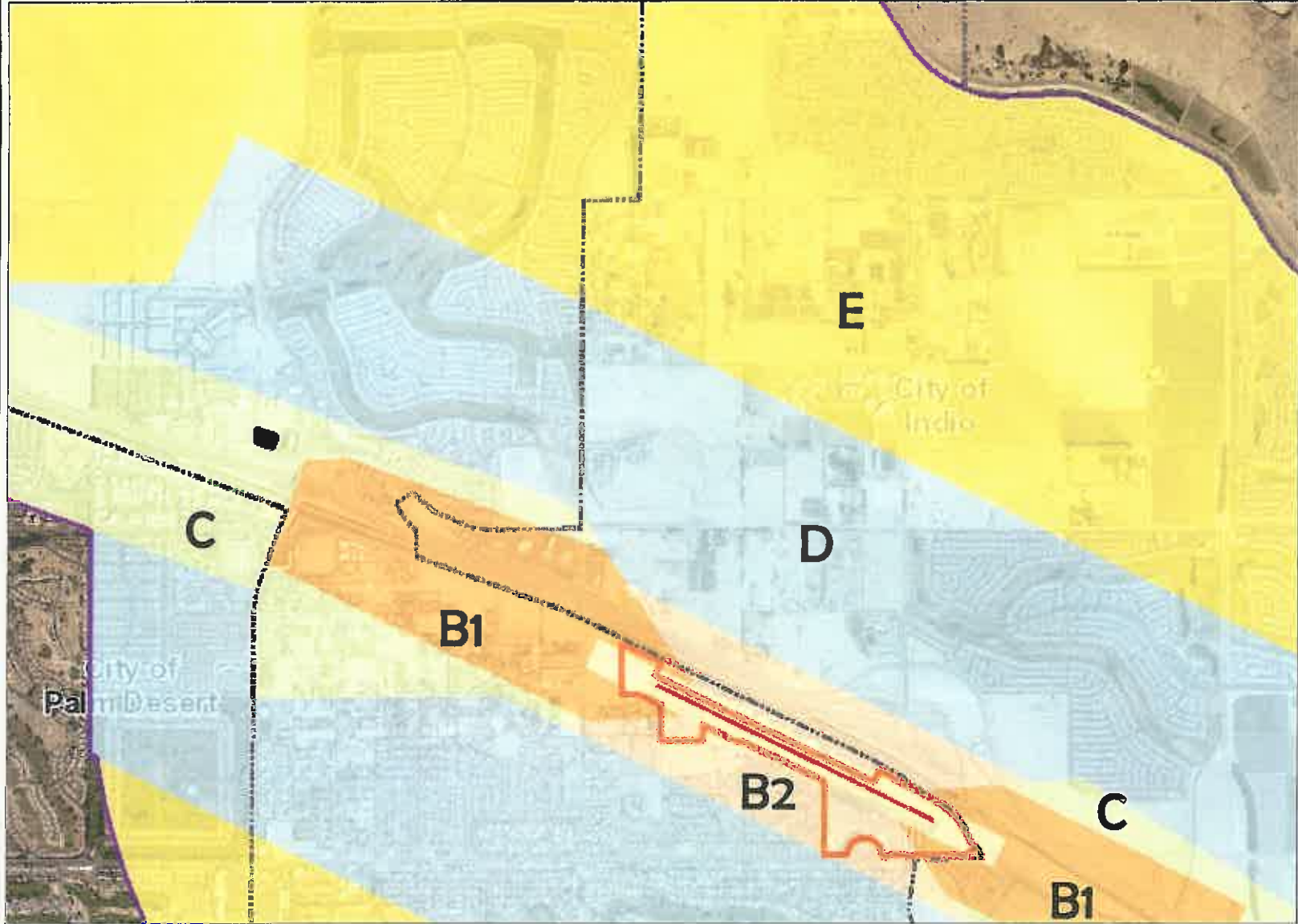
*(Adopted December 2004)*

Map BD-1

**Compatibility Map  
Bermuda Dunes Airport**



# Map My County Map



### Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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### Notes

# Map My County Map



- Legend**
- Parcels
  - Runways
  - Airports
  - Airport Influence Areas
  - Airport Compatibility Zones**
  - OTHER COMPATIBILITY ZONE
  - A
  - A-EXC1
  - B1
  - B1-APZ I
  - B1-APZ I-EXC1
  - B1-APZ II
  - B1-APZ II-EXC1
  - B1-EXC1
  - B2
  - B2-EXC1
  - C
  - C1
  - C1-EXC1
  - C1-EXC3
  - C1-EXC4
  - C1-HIGHT
  - C2
  - C2-EXC1
  - C2-EXC2
  - C2-EXC3
  - C2-EXC5



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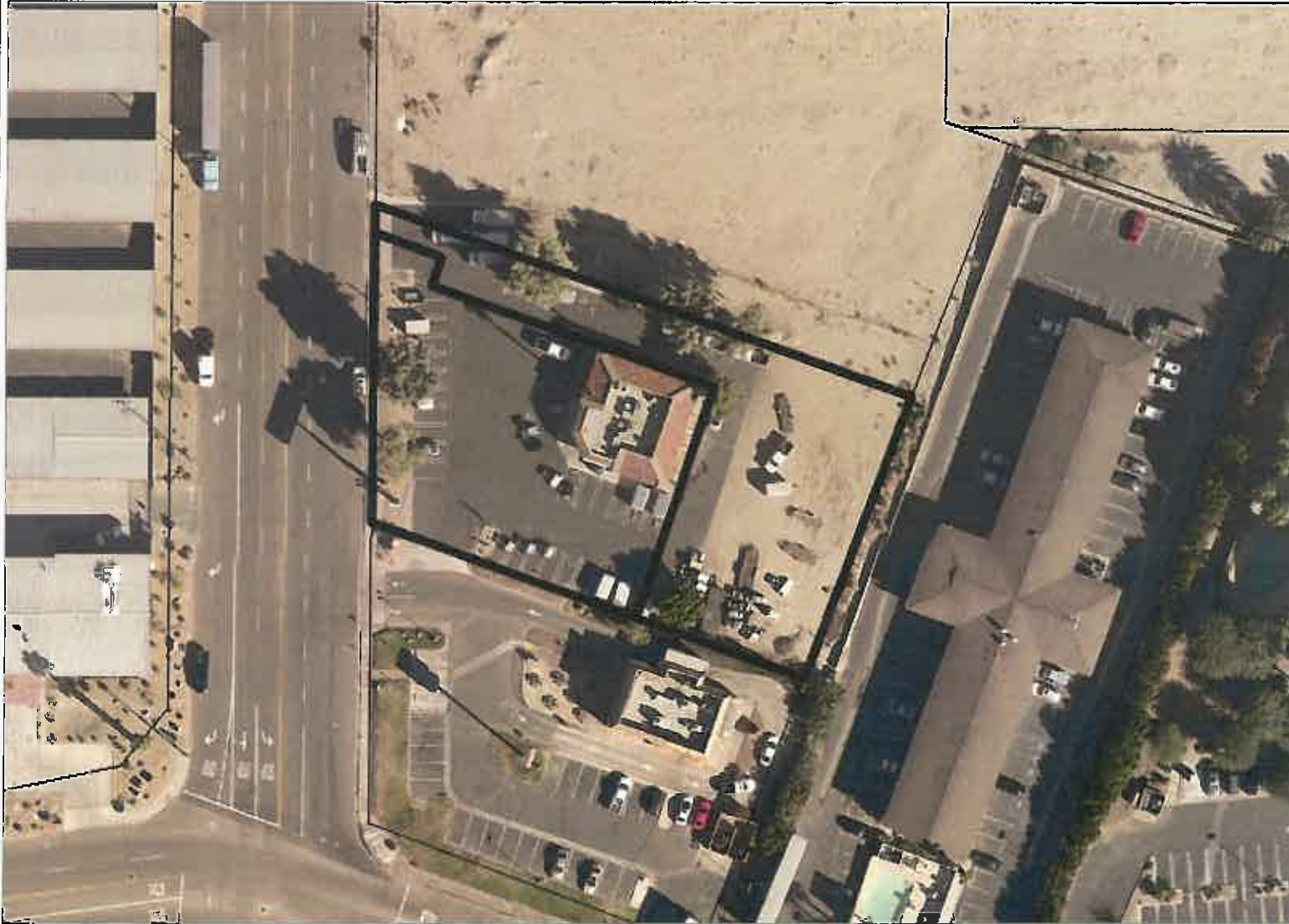


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**Notes**

# Map My County Map



## Legend

-  Parcels
-  Blueline Streams
-  City Areas
-  World Street Map



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## Notes

# Map My County Map



### Legend

- Blueline Streams
- City Areas
- World Street Map

### Notes



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# Map My County Map



## Legend

- Blueline Streams
- City Areas
- World Street Map



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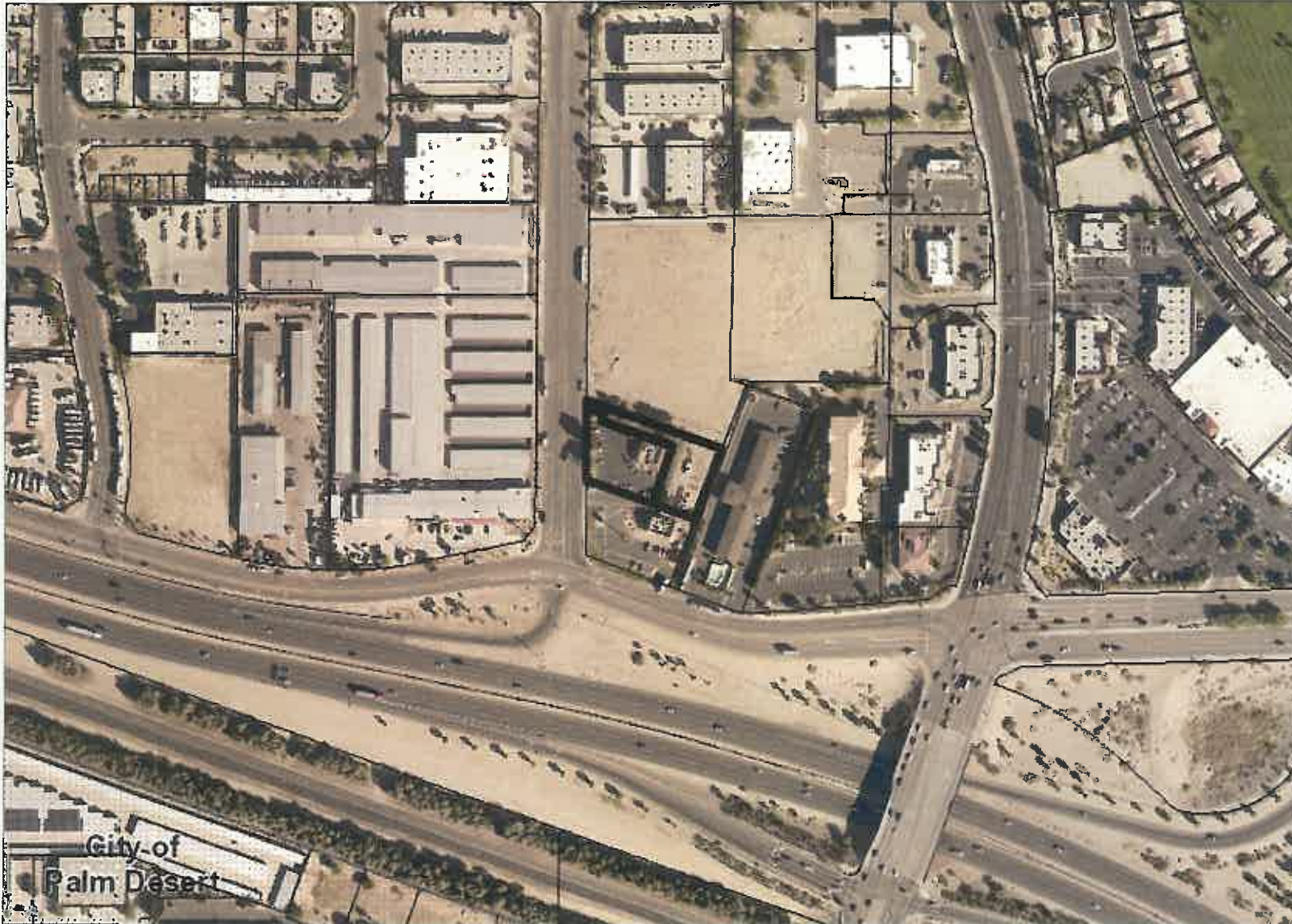
## Notes

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# Map My County Map



- Legend**
-  Parcels
  -  Blueline Streams
  -  City Areas
  -  World Street Map



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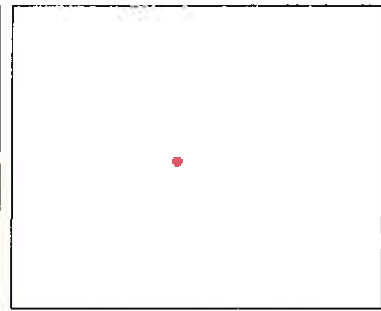
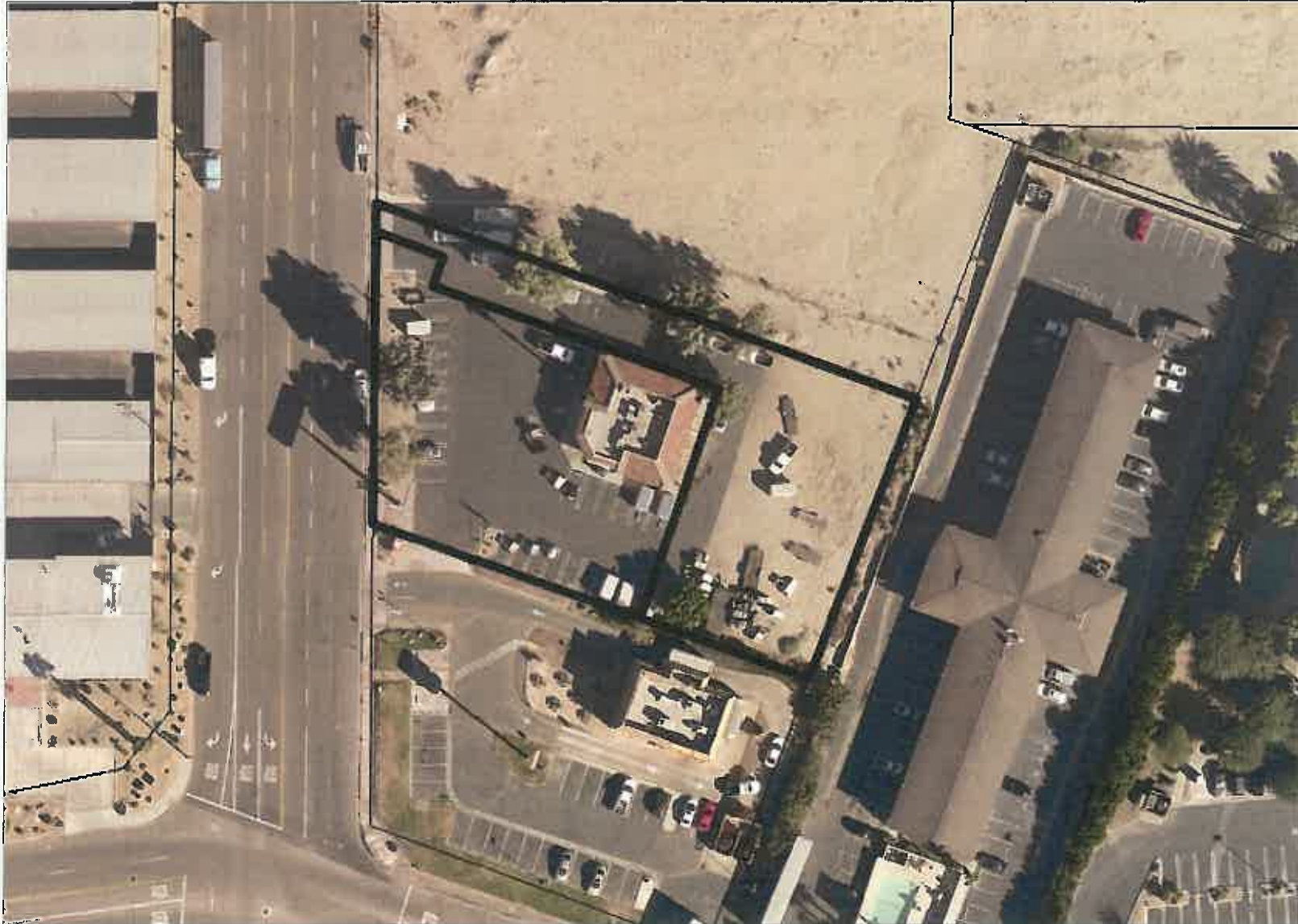
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**Notes**

# Map My County Map



## Legend

-  Parcels
-  Blueline Streams
-  City Areas
-  World Street Map



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## Notes

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NEW CONDITIONAL USE PERMIT FOR:

# F2 - PALM DESERT, LLC (CANNABIS STOREFRONT RETAILER)

39420 BERKEY DRIVE  
PALM DESERT, CA 92211

**CLIENT:**  
F2-PALM DESERT, LLC  
ALBERTO AVI MARCIANO  
24 HAMMOND  
IRVINE, CA 92618

**EMAIL:** jon@crechardhowell.com  
**PHONE:** 714-522-5553



## PROJECT SHEET INDEX

SHEET	SHEET TITLES
A1.00	PROJECT - COVER SHEET
A1.01	U.S. GEOLOGICAL SURVEY CALADRIANGLE MAP
A2.01	PHOTO-SHEET
A2.11	SITE PLAN
A3.11	PROPOSED FLOOR PLAN
A3.21	PROPOSED SECURITY FLOOR PLAN
A7.11	EXTERIOR ELEVATIONS
A7.22	EXTERIOR ELEVATIONS

**GRAPHIA**  
ARCHITECTURE  
ENGINEERING

100 GATEWAY DRIVE, SUITE 200  
IRVINE, CA 92618  
952-201-6800  
Helen@GRAPHIA.com  
HGRAPHIA.com

These drawings are instruments of service and are the property of Sean Frutkin, Architect. All design information contained on these drawings are for use only on this specified project and shall not be used elsewhere without the expressed written permission by Sean Frutkin, Architect.

PROJECT: 20190806.4

CLIENT:  
F2-PALM DESERT, LLC  
24 HAMMOND  
IRVINE, CA 92618  
PHONE: 714-522-5553

### VICINITY MAP



### PROJECT DIRECTORY

**ARCHITECT | ENGINEER**  
SEAN FRUTKIN ARCHITECT, INC. 62326 - IRVINE | (949) 522-5553 | SEAN@GRAPHIA.COM | SEAN@FRUTKIN.COM  
100 GATEWAY DRIVE, SUITE 200, IRVINE, CA 92618

### SITE SECURITY

DISPENSARY SITE SHALL COMPLY WITH THE FOLLOWING SECURITY REQUIREMENTS

A DISPENSARY PERMITTEE SHALL COMPLY WITH THE SECURITY PLAN THAT IS APPROVED BY THE CITY MANAGER, WHICH PLAN MUST INCLUDE BUILDING SPECIFICATIONS, LIGHTING, ALARMS, AND STATE-APPROVED SECURITY PERSONNEL.

EACH SECURITY PLAN APPROVED BY THE CITY MANAGER MUST INCLUDE THE FOLLOWING: 1. SECURITY SURVEILLANCE CAMERA, SECURITY SURVEILLANCE CAMERA AND A VIDEO RECORDING SYSTEM MUST BE INSTALLED TO MONITOR ALL DOORS INTO THE BUILDING ON THE DISPENSARY SITE, THE PARKING LOT, LOADING AREAS, AND ALL EXTERIOR SIDES OF THE PROPERTY ADJACENT TO THE PUBLIC SIDE OF THE LOT. THE CAMERA AND RECORDING SYSTEM MUST BE OF ADEQUATE QUALITY, COLOR RESOLUTION, AND RESOLUTION TO ALLOW THE IDENTIFICATION OF ANY INDIVIDUAL PRESENT ON THE DISPENSARY SITE. THE RECORDING SYSTEM MUST BE CAPABLE OF EXPORTING THE RECORDING VIDEO IN STANDARD MP40 FORMATS TO ANOTHER COMMON MEDIA, SUCH AS A DVD OR USB DRIVE. 2. SECURITY VIDEO RECORDING AND RETENTION. VIDEO FROM THE SECURITY SURVEILLANCE CAMERA MUST BE RECORDING AT ALL TIMES 24 HOURS A DAY, SEVEN DAYS A WEEK AND THE RECORDING SHALL BE MAINTAINED FOR AT LEAST 30 DAYS. THE VIDEO RECORDINGS SHALL BE MADE AVAILABLE TO THE CITY UPON REQUEST. 3. ALARM SYSTEM. PROFESSIONALLY AND CENTRALLY MONITORED FIRE, ROBBERY, AND BURGLAR ALARM SYSTEMS MUST BE INSTALLED AND MAINTAINED IN GOOD WORKING CONDITION. THE ALARM SYSTEM MUST INCLUDE A PRIVATE SECURITY COMPANY THAT IS REQUIRED TO RESPOND TO IT 24 HOURS A DAY.

A MARIJUANA DISPENSARY PERMITTEE SHALL REPORT TO THE CITY POLICE DEPARTMENT ALL CRIMINAL ACTIVITY OCCURRING ON THE DISPENSARY SITE.

### PERMIT DISPLAY

DISPLAY OF MARIJUANA DISPENSARY PERMIT

THE MARIJUANA DISPENSARY PERMITTEE SHALL DISPLAY ITS CURRENTLY VALID MARIJUANA DISPENSARY PERMIT ISSUED IN ACCORDANCE WITH THE ARTICLE NAMED THE LOBBY OR WAITING AREA OF THE MAIN ENTRANCE TO THE DISPENSARY SITE. THE PERMIT SHALL BE DISPLAYED AT ALL TIMES IN A CONSPICUOUS PLACE SO THAT IT MAY BE READILY SEEN BY ALL PERSONS ENTERING THE DISPENSARY SITE.

### SIGNAGE

DISPLAY OF MARIJUANA DISPENSARY SIGN

A MARIJUANA DISPENSARY PERMITTEE SHALL POST IN THE LOBBY OF THE DISPENSARY SITE SIGNS THAT MEET THE FOLLOWING: 1. SIGNS SHALL BE PROTECTED FROM INTERFERING WITH THE 1. "SMOOTHING, INTERFERING, OR CONSUMING MARIJUANA ON OR WITHIN 20 FEET OF THIS SITE IS PROHIBITED." 2. EACH SIGN DETERMINED IN DIMENSION A MUST BE AT LEAST 8 INCHES BY 30 INCHES IN SIZE AND MUST BE DISPLAYED AT ALL TIMES IN A CONSPICUOUS PLACE SO THAT IT MAY BE READILY SEEN BY ALL PERSONS ENTERING THE DISPENSARY SITE.

### PROJECT SCOPE

PROJECT SCOPE: PROVIDE ALL LABOR, MATERIALS, EQUIPMENT, TOOLS AND TRANSPORTATION AS REQUIRED FOR TENANT IMPROVEMENTS OF AN EXISTING BUILDING FOR COMMERCIAL CANNABIS NON-VOLATILE DISPENSARY INCLUDING DISPENSARY ROOM, WAITING ROOM, RESTROOM, AND OFFICE SPACE. WORK INCLUDES DEMOLITION, FRAMING, PLUMBING, MECHANICAL, ELECTRICAL, FIRE/SMOKEPROOF, WOODWORK, DOORS AND CASEWORK AND EQUIPMENT.

### DESIGN NARRATIVE

THE SCOPE FOR CONSTRUCTION WORK INCLUDES DEMOLISHING 2,592 SQUARE FEET OF EXISTING WOOD-FRAMED BUILDING LOCATED AT 39420 BERKEY DR, PALM DESERT, CA. INTERIOR SPACE DEMOLISH SHALL INCLUDE ACCESSIBILITY UPDATES, INTERIOR WALLS, ROOM DIVIDERS, PROGRESSIVE SPRINKLER, LIGHTING AND RELATED DATA, SECURITY, MECHANICAL, ELECTRICAL AND PLUMBING IMPROVEMENTS.

### ODOR CONTROL

DISPENSARY SITE SHALL COMPLY WITH THE FOLLOWING ODOR CONTROL REQUIREMENTS

A MARIJUANA DISPENSARY PERMITTEE SHALL PREVENT ALL ODORS GENERATED FROM THE DISPENSARY AND STORAGE OF MARIJUANA FROM ESCAPING FROM THE BUILDINGS ON THE DISPENSARY SITE, SUCH THAT THE ODOR CANNOT BE DETECTED BY A REASONABLE PERSON OF NORMAL SENSITIVITY OUTSIDE THE BUILDINGS.

THE ACTIVATED CARBON FILTRATION IS INSTALLED TO CONTROL ODORS FROM BEING EXTRACTED OUT OF PROHIBITORY OPERATIONS.

ACTIVATED CARBON FILTRATION TECHNIQUE INCLUDES EXHAUSTING THE AIR OUTSIDE WITHIN THE AIRC SYSTEM THROUGH AN ACTIVATED CARBON FILTER IN ORDER TO REMOVE ODORS AND PARTICULATES THAT MAY POSE A PUBLIC HEALTH RISK. EXHAUST AIRS SHOULD THROUGH ACTIVATED CARBON FILTER TO AVOID THE SMOG ASSOCIATED WITH DISPENSARY CANNABIS. FILTERED PRODUCT WILL BE STORED IN A SEALED CONTAINER TO AVOID OILY OILS AND PARTICULATES.

### BUSINESS OPERATIONS

CANNABIS BUSINESS WILL CARRY ADULT AND MEDICAL USE LICENSES.

BUSINESS SHALL BE A CANNABIS STOREFRONT RETAILER WITH DELIVERY. THE 16.

CANNABIS STOREFRONT RETAILER SHALL OPERATES FROM 9:00 AM TO 12:00 PM MONDAY THROUGH SUNDAY, AND BE OPEN TO THE PUBLIC FROM 1:00 AM TO 1:00 AM PM.

### APPLICABLE BUILDING CODES

- TITLE 17.8, CALIFORNIA CODE OF REGULATIONS, PUBLIC SAFETY, FIRE REGULATIONS
- 2018 CALIFORNIA ADMINISTRATIVE CODE, TITLE 24, PART 1
- 2018 CALIFORNIA BUILDING CODE (CBC), TITLE 24, PART 2
- 2018 CALIFORNIA ELECTRICAL CODE (CEC), TITLE 24, PART 3
- 2018 CALIFORNIA MECHANICAL CODE (CMC), TITLE 24, PART 4
- 2018 CALIFORNIA PLUMBING CODE (CPC), TITLE 24, PART 5
- 2018 CALIFORNIA ENERGY CODE, TITLE 24, PART 6
- 2018 CALIFORNIA FIRE CODE (FC), TITLE 24, PART 8
- 2018 CALIFORNIA GREEN BUILDING STANDARDS CODE (GBSC), TITLE 24, PART 11
- 2018 CALIFORNIA REFERENCED STANDARDS, TITLE 24, PART 12
- 2018 CBC, CHAPTER 35 AND/OR CBC CHAPTER 45 INCLUSIONS:
  - 2018 NFPA 101, SPANNER RETIARDER (CALIFORNIA AMENDMENT)
  - 2018 NFPA 24, FIRE SERVICE MANS (CALIFORNIA AMENDMENT)
  - 2018 NFPA 72, FIRE ALARM CODE (CALIFORNIA AMENDMENT)

### BUILDING AREA ANALYSIS

UNCONDITIONED AREA:	EXISTING AREA	NEW AREA	TOTAL AREA
	0.57	0.57	0.57
TOTAL UNCONDITIONED AREA:	0.57	0.57	0.57
CONDITIONED AREA:	EXISTING AREA	NEW AREA	TOTAL AREA
TENANT SPACE	2,592.57	0.57	2,572.57
	0.57	0.57	0.57
	0.57	0.57	0.57
	0.57	0.57	0.57
TOTAL CONDITIONED AREA:	2,592.57	0.57	2,592.57
TOTAL BUILDING AREA:	2,592.57	0.57	2,572.57
TOTAL BUILDING COVERAGE:			2,572.57
TOTAL SITE AREA:			22,780.57
TOTAL SITE COVERAGE AREA:			11.81 %

### EXTERIOR COLORS

ROOFING	NO CHANGE
OUTRIG	NO CHANGE
DOORS	NO CHANGE
WINDOWS	NO CHANGE
WALL BODY	NO CHANGE
WALL BODY TRIM	NO CHANGE

NEW CONDITIONAL USE PERMIT FOR:  
**F2 - PALM DESERT, LLC**  
(CANNABIS STOREFRONT RETAILER)

39420 BERKEY DRIVE  
PALM DESERT, CA 92211

CLIENT:  
F2-PALM DESERT, LLC  
ALBERTO AVI MARCIANO  
24 HAMMOND  
IRVINE, CA 92618  
PHONE: 714-522-5553



NEW CONDITIONAL USE PERMIT FOR:  
**F2 - PALM DESERT, LLC**  
(CANNABIS STOREFRONT RETAILER)

39420 BERKEY DRIVE  
PALM DESERT, CA 92211

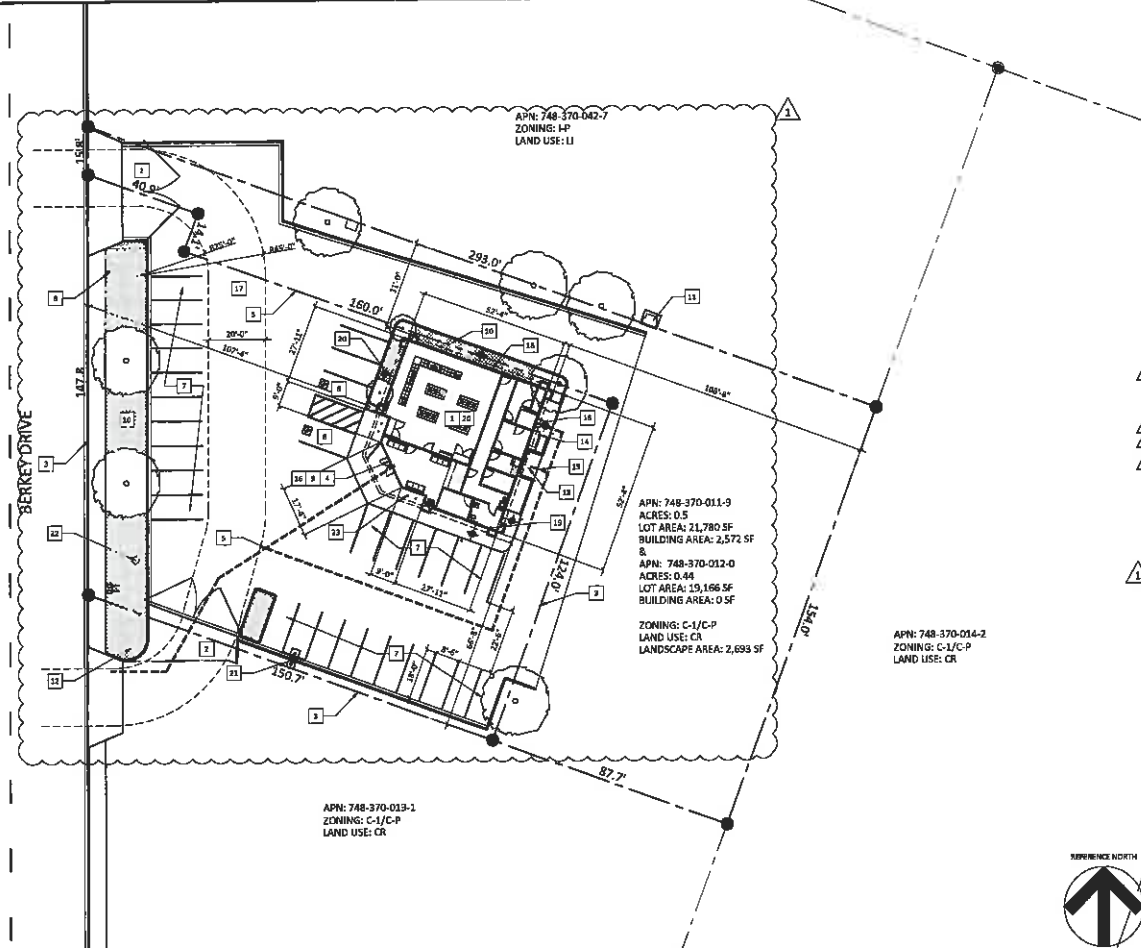
TITLES:  
PROJECT - COVER SHEET



THIS PROJECT IS NOT APPROVED FOR CONSTRUCTION UNTIL THE CITY HAS ALL SEALS AND SIGNS OBTAINED BY THE ARCHITECT AND THE DESIGN AUTHORITY HAS APPROVED IT.

SHEET:  
**A1.00**





### KEYNOTES

- NEW CANNABIS STOREFRONT RETAILER FACILITY.
- EXISTING DRIVEWAY.
- EXISTING PROPERTY LINE.
- EXISTING MAIN BUILDING ENTRANCE & EMPLOYEE ACCESS POINT.
- EXISTING ACCESSIBLE PATH OF TRAVEL TO PUBLIC WAY. EXISTING GRADE MEETS FLOOR REQUIREMENTS.
- EXISTING ACCESSIBLE PARKING SPACE.
- EXISTING PARKING AREA.
- EXISTING FIRE HYDRANT - APPROXIMATE LOCATION.
- NEW FIRE & LIFE SAFETY: EACH BUILDING'S MAIN ENTRANCE SHALL BE EQUIPPED WITH A 3200 LUMENS EMERGENCY LIGHTING.
- EXISTING LANDSCAPE AREA.
- EXISTING UTILITY ELECTRICAL.
- EXISTING ACCESSIBLE PARKING SIGN LOCATION.
- EXISTING TRASH / RECYCLE BIN.
- EXISTING UTILITY ELEC.
- EXISTING ACCESSIBLE PATH OF TRASH ENCLOSURE. EXISTING GRADE MEETS SLOPE REQUIREMENTS.
- EXISTING DOORS SHALL BE SOLID CORE AND SHALL BE EQUIPPED WITH A 300 GRADE FINISHING SCHEDULE TO SCREEN PERSONS BEFORE ALLOWING ENTRY.
- EXISTING FIRE DEPARTMENT ACCESS.
- EXISTING BUILDING PERIMETER.
- EXISTING DOMESTIC WATER BACK FLOW DEVICE.
- EXISTING EXTERIOR BUILDING LIGHTING. EXTERIOR LIGHTING SHALL BE 5 VOLT LIGHT USING LED LAMPS WITH FULL CUT-OFF FIXTURES TO LIMIT GLARE AND LIGHT PENETRATION. COLOR TEMPERATURE SHALL BE BETWEEN 2700K AND 3000K WITH A COLOR RENDERING INDEX OF 75 OR BETTER AND A LIGHT LOSS FACTOR OF 85 OR BETTER. EXTERIOR DRIVE AND PAVEMENT AND BIODEGRADABLE LIGHTING SHALL BE ILLUMINATED TO A MAINTAINED MINIMUM OF 3.0 FOOT CANDLES PER SQUARE FOOT OF PARKING AREA AT A 4:00 AVERAGE TO MAINTAIN PATH. EXTERIOR WALKWAYS, COFFERS AND PASSAGEWAYS SHALL BE ILLUMINATED TO A MAINTAINED MINIMUM OF 3.0 FOOT CANDLES PER SQUARE FOOT OF SURFACE AREA AT A 4:00 AVERAGE TO MAINTAIN PATH.
- EXISTING POLE MOUNTED LIGHTING - TO REMAIN.
- EXISTING FULCRUM SHALL BE REPLACED FOR "F3 PALM DESERT" BUSINESS SIGNAGE. NOT TO EXCEED 4'-0" IN HEIGHT - REFERENCE VIEWS P.10, P.11, P.12.
- NEW SITE BOLLARD.

### BUSINESS OPERATIONS

CANNABIS BUSINESS IS A LARRY ADULT AND MEDICAL USE LICENSE.

### SITE NOTES

STORM WATER: NEWLY CONTRACTED PROJECTS WHICH COVER LESS THAN ONE ACRE OF LAND SHALL PRESENT THE FOLLOWING OF STORM WATER RUNOFF FROM THE CONSTRUCTION ACTIVITIES THROUGHOUT THE CONSTRUCTION PERIOD:

- DESIGNATED PARKING: PROVIDE DESIGNATED PARKING FOR ANY COMBINATION OF LOW-EMISSION, FUEL-EFFICIENT, AND CARPOOL/VAN POOL VEHICLES AS SHOWN IN DETAIL 5.005.2.2. PARKING SHALL MAINTAIN SHALL COMPLY WITH CDDC 85.009.5.2.
- CONSTRUCTION WASTE OVERSEEN: A MINIMUM OF 65% OF THE NON-HAZARDOUS CONSTRUCTION AND DEMOLITION WASTE GENERATED AT THE SITE WILL BE DIVERTED TO AN OFFSITE RECYCLE, DIVERSION, OR SALVAGE FACILITY.
- MAIN ENTRANCE AND LOBBY: THE DISPENSARY SITE SHALL HAVE A BUILDING WITH A MAIN ENTRANCE THAT IS CLEARLY VISIBLE FROM THE PUBLIC STREET OR SIDEWALK. THE MAIN ENTRANCE SHALL BE UNHINDERED CLEAR OF BARBERS, LANDSCAPING, AND OTHER OBSTRUCTIONS INSIDE OF THE MAIN ENTRANCE. THERE SHALL BE A LOBBY TO RECEIVE PERSONS TO THE SITE AND TO VERIFY WHETHER THEY ARE ALLOWED IN THE DISPENSARY AREAS.
- DISPENSARY: ALL SECURE DISPENSARY AREAS IN ANY BUILDING ON THE DISPENSARY SITE SHALL BE SEPARATED FROM THE MAIN ENTRANCE AND LOBBY, AND SHALL BE SECURED BY A LOCK ACCESSIBLE ONLY TO MANAGERS AND STAFF OF THE MAJORANA DISPENSARY PERMITTED.

### SITE PARKING COUNT

	PARKING REQUIRED	PARKING PROVIDED
TOTAL PARKING (M-1): 1 PER 200 SF:	23 SPACES	80 SPACES

### SECURITY NOTES

IN THE EVENT THAT ANY PORTION OF THE ALARM SYSTEM IS INOPERABLE, THE FACILITY SHALL BE STAFFED AT ALL TIMES UNTIL THE ALARM SYSTEM IS FULLY FUNCTIONAL.

RECORDED VIDEO ASSESSMENT AND SURVEILLANCE SYSTEM (VMS) SHALL BE EMPLOYED.

CAMERAS AND VMS STORAGE SHALL BE DIGITAL HIGH DEFINITION OR BETTER, SET TO HIGH QUALITY SETTINGS.

VMS STORAGE SHALL BE KEPT OFF-SITE OR IN A SECURED AREA ACCESSIBLE ONLY TO MANAGEMENT.

VMS SHALL BE EQUIPPED WITH A BATTERY BACK-UP WITH AT LEAST 24 HOURS OF STANDBY TIME IN CASE OF POWER FAILURE.

VMS SHALL SUPPORT STANDARD MPER FORMATS.

VMS SHALL BE CAPABLE OF STORING NO LESS THAN 30 DAYS WORTH OF ACTIVITY AT HIGH QUALITY SETTINGS.

MANAGER WITH ACCESS TO VMS STORAGE SHALL BE ABLE TO RESPOND WITHIN ONE HOUR. MANAGER SHALL HAVE THE ABILITY TO TRANSMIT RECORDED DATA TO ANOTHER MEDIUM (E.G. DVD, FLUORID BURNER, ETC.) CAMERAS SHALL BE EQUIPPED WITH IR LEDS, CAPABILITY, AUDIO REC AND AUTO FOCUS. MONITORING DISPLAYING THE EMPLOYEE PARKING AREA AND PROPERTY PERMITTER SHALL BE EMPLOYED IN A VISIBLE LOCATION FROM THE FRONT WALKWAY AND EMPLOYEES WILL BE REQUIRED TO REPORT ANY THAT IMPROVES MAY MONITOR THE OUTSIDE ENVIRONMENT PRIOR TO DEPARTING OFFICE, PARKING LOT, LOADING AREA COVERAGE OF NORTH AND SOUTH EXTERIOR SIDES OF THE FACILITY.

NO MORE THAN 10% PERCENT OF THE SQUARE FOOTAGE OF THE WINDOWS AND GLASS DOORS SHALL BE OCCUPIED BY ADVERTISING, SIGNS, SHIELDS OR ANYTHING ELSE. ALL ADVERTISING, SIGNS, AND SHIELDS SHALL BE PLACED AND MAINTAINED IN A MANNER THAT ENURES THAT LANY EMPLOYEES PERSONNEL HAVE A CLEAR AND UNOBSTRUCTED VIEW OF THE INTERIOR OF THE PREMISES FROM THE EXTERIOR PUBLIC SIDEWALK OR ENTRANCE TO THE PREMISES. ALL SIGNS SHALL COMPLY WITH THE CITY CODE.

NO PUBLIC PAY PHONES/CELL PHONES SHALL BE ALLOWED ON THE PREMISES.

NO COIN OPERATED BARBERS OR VIDEO MACHINES SHALL BE ALLOWED ON THE PREMISES.

THE APPLICANT IS RESPONSIBLE FOR REASONABLY CONTROLLING THE CONDUCT OF PERSONS ON THE SITE AND SHALL IMMEDIATELY DISPERSE LOITERERS.

ALL DUMPSTERS SHALL BE OFFLY LOCATED.

### SITE PLAN

SCALE: 1" = 30'

### ORDINANCE NO. 348

SECTION 18.4

- FREE-STANDING SIGN:
  - LOCATED WITHIN 400 FEET OF THE NEAREST EDGE OF A PRIMARY FRONT OF WAY LINE.
  - THE MAXIMUM HEIGHT OF A SIGN SHALL NOT EXCEED 45 FEET.
  - THE MAXIMUM SURFACE AREA OF A SIGN SHALL NOT EXCEED 150 SQUARE FEET.
- SHOPPING CENTERS - ALL LOCATIONS:
  - THE MAXIMUM SURFACE AREA OF A SIGN SHALL NOT EXCEED 50 SQUARE FEET OR 0.25 PERCENT OF THE TOTAL EXISTING BUILDING FLOOR AREA IN A SHOPPING CENTER, WHICHEVER IS GREATER, EXCEPT THAT IN ANY CASE, NO SIGN SHALL EXCEED 500 SQUARE FEET IN SURFACE AREA.
  - THE MAXIMUM HEIGHT OF A SIGN SHALL NOT EXCEED 16 FEET.
- ALL OTHER LOCATIONS:
  - THE MAXIMUM SURFACE AREA OF A SIGN SHALL NOT EXCEED 50 SQUARE FEET.
  - THE MAXIMUM HEIGHT OF A SIGN SHALL NOT EXCEED 16 FEET.
- NUMBER OF FREE-STANDING SIGNS - ALL LOCATIONS:
  - NOT MORE THAN ONE FREE-STANDING SIGN SHALL BE PERMITTED ON A PARCEL OF LAND, EXCEPT THAT IF A SHOPPING CENTER HAS FRONTAGE ON TWO OR MORE STREETS, THE SHOPPING CENTER SHALL BE PERMITTED TWO FREE-STANDING SIGNS, PROVIDED THAT THE TWO SIGNS ARE NOT LOCATED ON THE SAME STREET ARE AT LEAST 300 FEET APART AND THE SECOND SIGN DOES NOT EXCEED 150 SQUARE FEET IN SURFACE AREA AND 16 FEET IN HEIGHT.

### GRAPHIA ARCHITECTURE & ENGINEERING

100 GATEWAY DRIVE, SUITE 220  
LINCOLN, CA 95648  
916.209.9016  
info@graphia.com  
www.graphia.com

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PROJECT: 20190806.4

CLIENT:  
F2 - PALM DESERT, LLC  
24 - HANFORD  
18 - BERKELEY, CA 94704  
PHONE: 714.933.5435

NEW CONDITIONAL USE PERMIT FOR:  
**F2 - PALM DESERT, LLC**  
**(CANNABIS STOREFRONT RETAILER)**  
39420 BERKEY DRIVE  
PALM DESERT, CA 92211

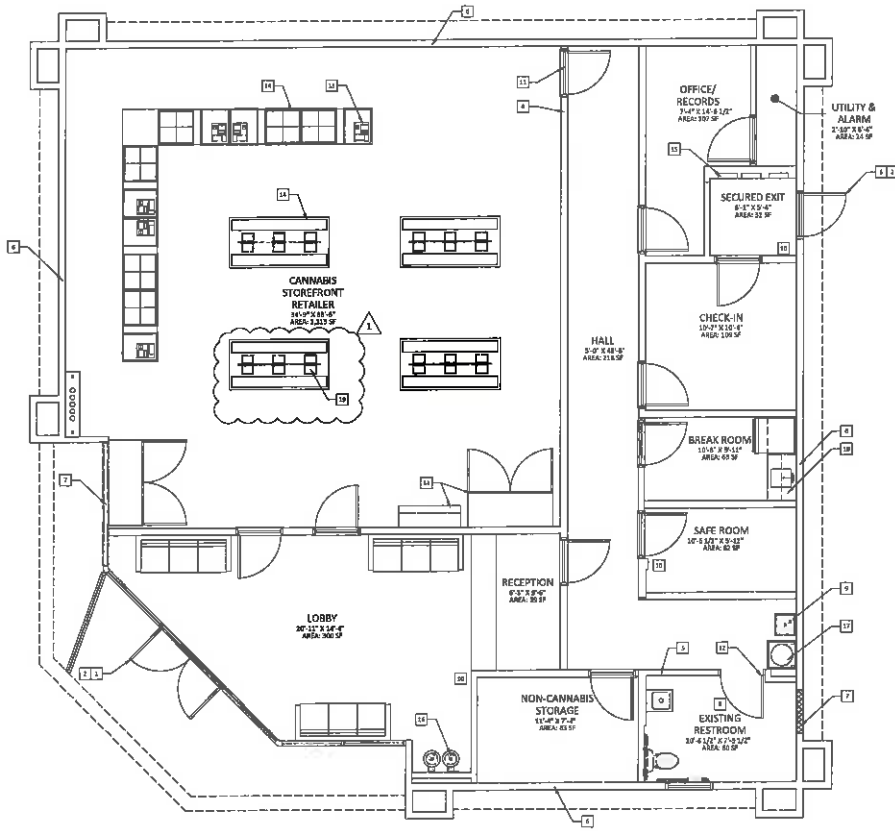
TITLES:  
SITE PLAN

DATE: 04/20/2023 10:11 AM

ARCHITECT'S APPROVAL:

THIS PROJECT CANNOT BE APPROVED FOR CONSTRUCTION UNTIL THE APPLICANT HAS OBTAINED ALL NECESSARY PERMITS AND THE BUILDING ACTIVITY SHALL BE IN ACCORDANCE WITH THE CITY CODE.

SHEET:  
**A2.11**



### KEYNOTE LEGEND

1	EXISTING MAIN BUILDING ENTRANCE & EMPLOYEE ACCESS POINT; EXISTING MAIN BUILDING ENTRANCE & EMPLOYEE ACCESS POINT; ALL SOLID CORE EXTERIOR DOORS SHALL BE EQUIPPED WITH 180 DEGREE VIEWING DEVICE TO SCREEN PERSONS BEFORE ALLOWING ENTRY. REFERENCE DOOR SCHEDULE.
2	EXISTING LEVEL LANDING.
3	EXISTING EXTERIOR DOOR ALL SOLID CORE EXTERIOR DOORS SHALL BE EQUIPPED WITH 180 DEGREE VIEWING DEVICE TO SCREEN PERSONS BEFORE ALLOWING ENTRY. REFERENCE DOOR SCHEDULE.
4	INTERIOR PARTITION WALL.
5	EXISTING INTERIOR PARTITION WALL.
6	EXISTING EXTERIOR WALL.
7	EXTERIOR WALL INFILL.
8	ACCESSIBLE RESTROOM.
9	EXISTING UTILITY SINK.
10	HOLD-UP ALARM SYSTEM SHALL BE EMPLOYED NEAR THE LOBBY EMPLOYEE ENTRANCE AND SAFETY HOLD-UP ALARMS SHALL CAUSE THE DISPATCH OF THE PALM DESERT POLICE DEPARTMENT.
11	INTERIOR DOOR.
12	EXISTING INTERIOR DOOR.
13	POINT OF SALE.
14	SECURE PRODUCT DISPLAYS.
15	EXISTING ELECTRICAL PANEL.
16	ACCESSIBLE DRINKING FOUNTAIN.
17	EXISTING WATER HEATER.
18	COUNTERTOP AND CABINETS WITH SINK. PROVIDE 1/2" GIP WALL TO CONTAINMENT OF SINK WITH HIGH CH GROUT SURFACE NOT TO EXCEED 3/4" A.F.F. AND PARALLEL APPROACH OF 48"X6" CLEAR ON RANGE OR STOVE TOP ARE NOT F.P. 1000. UPPER CABINETS SHOWN DASHED WHERE COOKING - PROVIDED BY TENANT.
19	DESIGNATED AREA FOR RETAIL CALLERS OF INCIDENTAL GOODS. TO BE LESS THAN 10% OF THE CANNABIS RETAIL FLOOR AREA.

**GRAPHIA**  
ARCHITECTURE  
RENDERING

100 GATEWAY DRIVE, SUITE 120  
LINCOLN, CA 95648  
(916) 209-8880  
Design @ GRAPHIA.com  
GRAPHIA.com

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PROJECT: 20190806.4

CLIENT:  
F2 PALM DESERT, LLC  
28 HANCOCK  
PALM DESERT, CALIFORNIA  
PHONE: 760.921.5133

NEW CONDITIONAL USE PERMIT FOR:  
**F2 - PALM DESERT, LLC**  
**(CANNABIS STOREFRONT RETAILER)**  
39420 BERKELEY DRIVE  
PALM DESERT, CA 92211

TITLES  
PROJECT - FLOOR PLAN

DATE: 11.23.2019

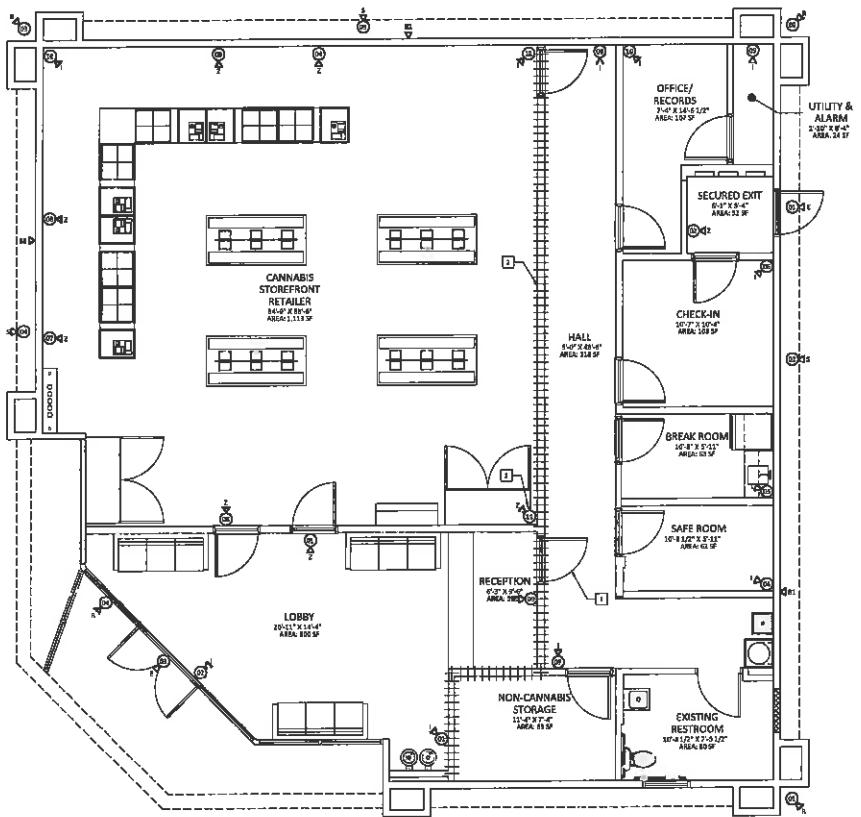
ARCHITECT'S APPROVAL:

*(Signature)*  
SEAN FRETZ, ARCHITECT  
100 GATEWAY DRIVE, SUITE 120  
LINCOLN, CALIFORNIA 95648  
PHONE: (916) 209-8880

THIS PROJECT IS MY APPROVED FOR CONSTRUCTION UNDER THE CALIFORNIA ARCHITECTURE ACT AND THE ARCHITECTURE BOARD OF THE BUILDING AUTHORITY HAVING SUPERVISION.

SHEET:  
**A3.11**

DATE: 11.23.2019



### KEYNOTE LEGEND

- 1 LIMITED ACCESS DOOR
- 2 LIMITED ACCESS PERIMETER LINE
- 3 SECURITY CAMERAS

### KEYNOTES - SECURITY

- CANNABIS SHALL BE CANNIBIS CANNABLE WITH A RESOLUTION OF NO LESS THAN TWO (2) 4K PIXELS AND A MINIMUM FRAME RATE OF 15 FRAMES PER SECOND.
- 100 R CAMERA - AODP
- 100 R000 ROOF LASER BEAM.
- 100 S CAMERA - SURROUNDING.
- 100 E CAMERA - EXTERIOR ENTRANCE/EXIT PAN-TILT-ZOOM.
- 100 I CAMERA - INTERIOR ENTRANCE/EXIT PAN-TILT-ZOOM.
- 100 I1 CAMERA - INTERIOR PAN-TILT-ZOOM.

### SECURITY NOTES

IN THE EVENT THAT ANY PORTION OF THE ALARM SYSTEM IS INOPERABLE, THE FACILITY SHALL BE STAFFED AT ALL TIMES UNTIL THE ALARM SYSTEM IS FULLY FUNCTIONAL.

RECORDED VIDEO ASSESSMENT AND SURVEILLANCE SYSTEM (VASS) SHALL BE EMPLOYED.

CAMERAS AND VASS STORAGE SHALL BE DIGITAL HIGH DEFINITION OR BETTER, SET TO HIGH QUALITY SETTINGS.

VASS STORAGE SHALL BE REDUNDANT ON A SECURED AREA ACCESSIBLE ONLY TO MANAGEMENT.

VASS SHALL BE EQUIPPED WITH A BATTERY BACK-UP WITH AT LEAST 24 HOURS OF STAND-BY TIME IN CASE OF POWER FAILURE.

VASS SHALL SUPPORT STANDARD MP4S FORMATS.

VASS SHALL BE CAPABLE OF STORING NO LESS THAN 30 DAYS' WORTH OF ACTIVITY AT HIGH QUALITY SETTINGS.

MANAGER WITH ACCESS TO VASS STORAGE SHALL BE ABLE TO RESPOND WITHIN ONE HOUR.

MANAGER SHALL HAVE THE ABILITY TO TRANSFER RECORDED DATA TO ANOTHER MEDIUM (E.G. DVD, FLASH DRIVE, ETC.). CAMERAS SHALL BE EQUIPPED WITH LOW LIGHT CAPABILITY, AUTO IRIS AND AUTO FOCUS. MONITORS DISPLAYING THE EMPLOYER PARKING AREA AND PROPERTY PERIMETER SHALL BE LOCATED IN A VISIBLE LOCATION NEAR THE DOOR FROM WHICH EMPLOYEES WILL LEAVE AND DEPART SO THAT EMPLOYEES MAY MONITOR THE OUTSIDE ENVIRONMENT PRIOR TO DEPARTING THE FACILITY. VASS SHALL PROVIDE COMPREHENSIVE COVERAGE OF: PARKING, AREAS OF ENTRY AND EXIT, PARKING LOT, LOADING AREAS COVERAGE OF NORTH AND SOUTH EXTERIOR SIDES OF THE PROPERTY.

THE APPLICANT IS RESPONSIBLE FOR MANAGERIALLY CONTROLLING THE CONDUCT OF PERSONS ON THE SITE AND SHALL IMMEDIATELY EXPULS LOITERERS.

**GRAPHIA**  
ARCHITECTURE  
& ENGINEERING

100 GATEWAY DRIVE, SUITE 220  
IRVINE, CA 92618  
TEL: 949-899-8890  
info@graphia.com  
graphia.com

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PROJECT: 20190806.4

CLIENT:  
F2 PALM DESERT, LLC  
25100 BERKELEY DRIVE  
PALM DESERT, CA 92211  
PHONE: 714.922.3303

NEW CONDITIONAL USE PERMIT FOR:  
**F2 - PALM DESERT, LLC**  
(CANNABIS STOREFRONT RETAILER)  
39420 BERKELEY DRIVE  
PALM DESERT, CA 92211

TITLE:  
PROJECT - SECURITY FLOOR PLAN

DATE: 04/14/2020 10:47:20 AM

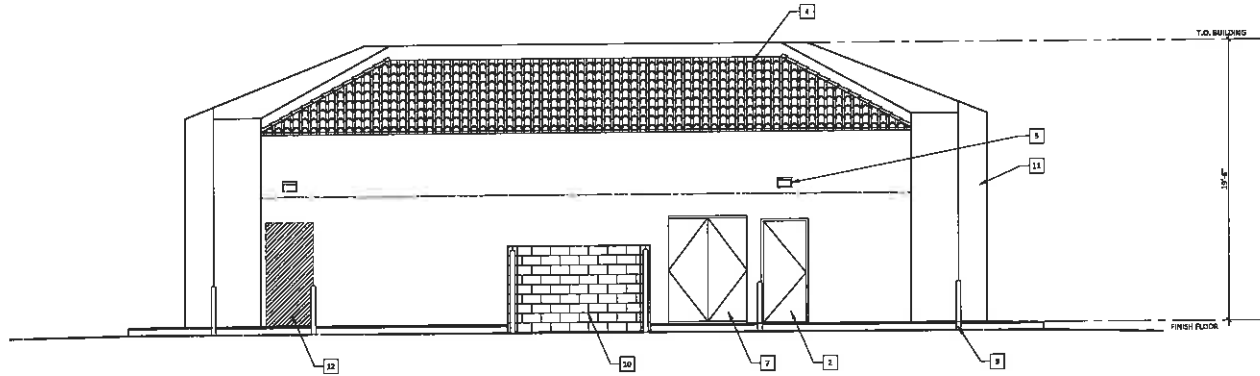
ARCHITECT'S APPROVAL:

SEAN FREITAS  
ARCHITECT  
100 GATEWAY DRIVE, SUITE 220  
IRVINE, CA 92618

THIS PROJECT IS NOT APPROVED BY THE COUNTY PLANNING DIVISION. THE COUNTY PLANNING DIVISION HAS REVIEWED THE ARCHITECT'S SITE PLAN AND HAS ISSUED A PERMIT TO CONSTRUCT. THE PLANNING DIVISION AUTHORITY IS LIMITED TO THE ARCHITECT'S SITE PLAN.

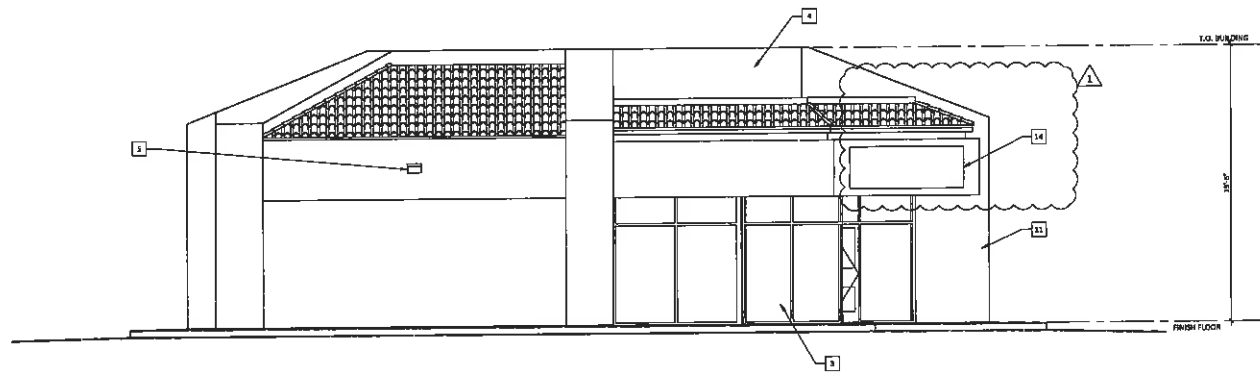
SHEET:  
**A3.21**

DATE: 07 19 2019



EXISTING ELEVATION: EAST

SCALE: 0" 1" 2" 4" 8" 2



EXISTING ELEVATION: WEST

SCALE: 0" 1" 2" 4" 8" 1

KEYNOTES

- 1 EXISTING MAIN BUILDING ENTRANCE.
- 2 EXISTING EMPLOYEE ACCESS POINT.
- 3 EXISTING STORE-FRONT.
- 4 EXISTING TILE ROOFING SYSTEM.
- 5 EXISTING LIGHTING.
- 6 EXISTING OFFICER DOOR ABANDON IN PLACE.
- 7 EXISTING SERVICE CABINET.
- 8 EXISTING BUILDING ADDRESS SIGNAGE, STREET/SUITE ADDRESS IS REQUIRED ON THE BUILDING THAT COMPLIES WITH CFC 908.
- 9 EXISTING STEEL BOLLARD.
- 10 EXISTING CAME ENCLOSURE WITH STEEL BOLLARD.
- 11 EXISTING STUCCO FINISH.
- 12 EXISTING STUCCO FINISH.
- 13 EXISTING STUCCO FINISH.
- 14 NEW "NO LITTERING" SIGN TO BE ACQUIRED FROM THE SHERIFF'S DEPARTMENT.
- 15 NEW "P-3 PALM DESERT" BUSINESS SIGNAGE AFFIXED TO BUILDING.

ORDINANCE NO. 348

SECTION 28.A

8. SIGNS APPLIED TO BUILDINGS - ALL AREAS.
1. NO ON-SITE ADVERTISING SIGN SHALL BE AFFIXED ON, ABOVE OR OVER THE ROOF OF ANY BUILDING, AND NO CHANGES ADVERTISING SHALL BE AFFIXED TO THE WALL OF A BUILDING SO THAT IT PROJECTS ABOVE THE PARAPET OF THE BUILDING. FOR THE PURPOSES OF THIS SECTION, A HANGING STYLE SIGN SHALL BE CONSIDERED A PARAPET.
  2. THE MAXIMUM SURFACE AREA OF SIGNS AFFIXED TO A BUILDING SHALL BE AS FOLLOWS:
    - a. FRONT WALL OF BUILDING - THE SURFACE AREA OF THE SIGN SHALL NOT EXCEED TEN PERCENT OF THE SURFACE AREA OF THE FRONT FACE OF THE BUILDING.
    - b. SIDE WALLS OF A BUILDING - THE SURFACE AREA OF THE SIGN SHALL NOT EXCEED TEN PERCENT OF THE SURFACE AREA OF THE SIDE FACE OF THE BUILDING.
    - c. REAR WALL OF A BUILDING - THE SURFACE AREA OF THE SIGN SHALL NOT EXCEED FIVE PERCENT OF THE SURFACE AREA OF THE REAR FACE OF THE BUILDING.

**GRAPHIA**  
ARCHITECTURE  
ENGINEERING

320 SATTWAY DRIVE, SUITE 120  
IRVINE, CA 92618  
(949) 269-9110  
design@graphia.com  
GRAPHIA.com

These drawings are the property of Graphia Architecture & Engineering, Inc. and are the property of Sean Frezza, Architect. All design information contained in these drawings are for use only on the specified project and shall not be used otherwise without the expressed written permission by Sean Frezza, Architect.

PROJECT: 20190806 4

CLIENT:  
F2 PALM DESERT, LLC  
28 HARBORVIEW  
IRVINE, CALIFORNIA  
PHONE: 714.922.5039

NEW CONDITIONAL USE PERMIT FOR:  
**F2 - PALM DESERT, LLC**  
(CANNABIS STOREFRONT RETAILER)  
3942D BERKEY DRIVE  
PALM DESERT, CA 92211

TITLES:  
EXTERIOR ELEVATIONS

DATE: 04/20/2019

ARCHITECT'S APPROVAL:

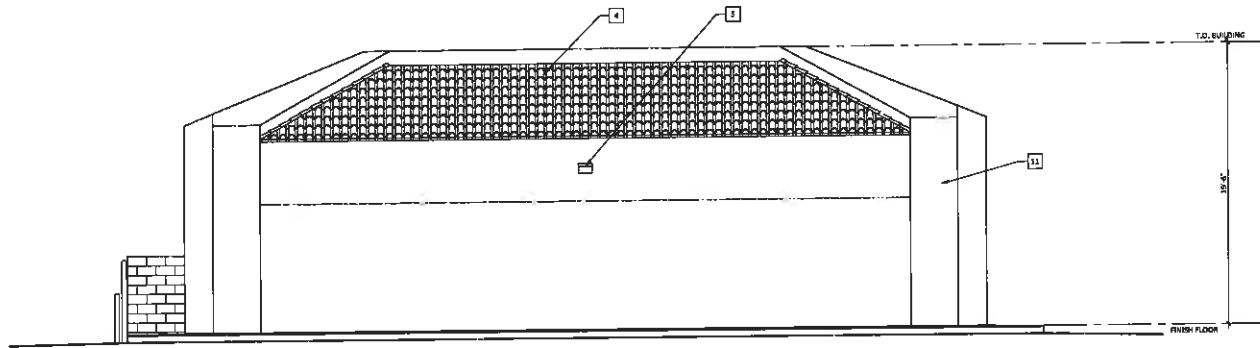
THIS PROJECT IS NOT APPROVED FOR CONSTRUCTION UNTIL THE OWNER HAS OBTAINED ALL NECESSARY PERMITS FROM THE BUILDING AUTHORITY AND THE BUILDING AUTHORITY HAS ISSUED APPROVAL.

SHEET:  
**A7.11**

PL 11 of 14

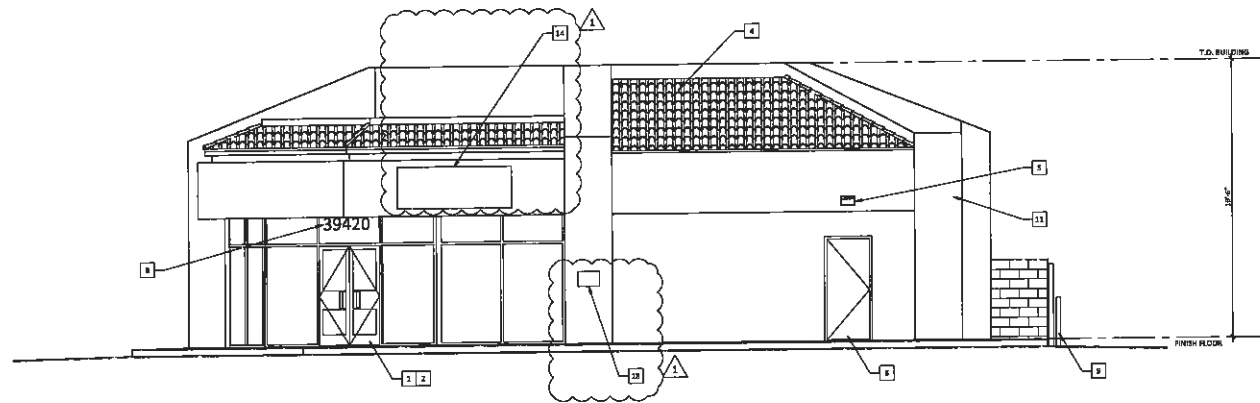
EXTERIOR COLORS

ROOFING	NO CHANGE.
SHUTTER	NO CHANGE.
DOORS	NO CHANGE.
WINDOWS	NO CHANGE.
WALL BODY	NO CHANGE.
WALL BODY TRIM	NO CHANGE.



EXISTING ELEVATION: NORTH

SCALE: 0' 1" 2" 4" 8' 2



EXISTING ELEVATION: SOUTH

SCALE: 0' 1" 2" 4" 8' 1

KEYNOTES

- 1 EXISTING MAIN BUILDING ENTRANCE.
- 2 EXISTING EMPLOYEE ACCESS POINT.
- 3 EXISTING STONE FRONT.
- 4 EXISTING TILE ROOFING SYSTEM.
- 5 EXISTING LIGHTING.
- 6 EXISTING EXTERIOR DOOR ABANDON IN PLACE.
- 7 EXISTING SERVICE CABINET.
- 8 EXISTING BUILDER ADDRESS SIGNAGE. STREET/ROUTE ADDRESS IS REQUIRED ON THE BUILDING THAT COMPLIES WITH CPC 305.
- 9 EXISTING STEEL BOLLARDS.
- 10 EXISTING CHAIN ENCLOSURE WITH STEEL BOLLARDS.
- 11 EXISTING TITLED TRIM.
- 12 EXTERIOR EXTERIOR WALL SHALL BE MAINTAINED EXISTING.
- 13 NEW "NO LETTERS" SIGN TO BE ADDED FROM THE SHERIFF'S DEPARTMENT.
- 14 NEW "F-2 PALM DESERT" BUSINESS SIGNAGE AFFIXED TO BUILDING.

ORDINANCE NO. 348

SECTION 19.4

6. SIGNS AFFIXED TO BUILDINGS - ALL AREAS.
1. NO ON-SITE ADVERTISING SIGNS SHALL BE AFFIXED ON, ABOVE OR OVER THE ROOF OF ANY BUILDING, AND NO ON-SITE ADVERTISEMENTS SHALL BE AFFIXED TO THE WALL OF A BUILDING UNLESS IT PROJECTS ABOVE THE PARAPET OF THE BUILDING. FOR THE PURPOSES OF THIS SECTION, A MANSARD STYLE ROOF SHALL BE CONSIDERED A PARAPET.
  2. THE MAXIMUM SURFACE AREA OF SIGNS AFFIXED TO A BUILDING SHALL BE AS FOLLOWS:
    - A. FRONT WALL OF BUILDING - THE SURFACE AREA OF THE SIGN SHALL NOT EXCEED TEN PERCENT OF THE SURFACE AREA OF THE FRONT FACE OF THE BUILDING.
    - B. SIDE WALL OF A BUILDING - THE SURFACE AREA OF THE SIGN SHALL NOT EXCEED TEN PERCENT OF THE SURFACE AREA OF THE SIDE FACE OF THE BUILDING.
    - C. REAR WALL OF A BUILDING - THE SURFACE AREA OF THE SIGN SHALL NOT EXCEED FIVE PERCENT OF THE SURFACE AREA OF THE REAR FACE OF THE BUILDING.

GRAPHIA  
ARCHITECTURE  
ENGINEERING

100 GATEWAY DRIVE, SUITE 120  
LINCOLN, CA 95646  
916.333.8996  
Dale@GRAPHIA.com  
GRAPHIA.com

These drawings are instruments of service and are the property of Sean Fretz, Architect. All design information contained on these drawings are for use only on this specified project and shall not be used otherwise without the expressed written permission by Sean Fretz, Architect.

PROJECT: 20190806.4

CLIENT:  
PALM DESERT, LLC  
10000 CALIFORNIA  
20 HANFORD  
HANFORD, CALIFORNIA  
PHONE: 760.922.2529

NEW CONDITIONAL USE PERMIT FOR:  
**F2 - PALM DESERT, LLC**  
(CANNABIS STOREFRONT RETAILER)  
39420 BERKEY DRIVE  
PALM DESERT, CA 92211

TITLES:  
EXTERIOR ELEVATIONS

ARCHITECT'S APPROVAL:



THIS PROJECT IS NOT APPROVED FOR CONSTRUCTION UNTIL THE TRIGGER DATE IS STAMPED AND SET FORTH BY THE APPLICANT AND THE BUILDING AUTHORITY HAS REVIEWED IT.

EXTERIOR COLORS

ROOFING	NO CHANGE.
GUTTER	NO CHANGE.
DOORS	NO CHANGE.
WINDOWS	NO CHANGE.
WALL BODY	NO CHANGE.
WALL BODY TRIM	NO CHANGE.

SHEET:

A7.12

DATE: OF -- SHEET



VIEW-P1



VIEW-A



VIEW-B



VIEW-C



VIEW-P2



VIEW-D



VIEW-E



VIEW-F



VIEW-P3



PROPERTY PHOTO MAP INDEX

ⓐ = CAMERA PLACEMENT AND PHOTO REFERENCE

**GRAPHIA**  
ARCHITECTURE  
& ENGINEERING

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graphia.com

These drawings are instruments of service and are the property of Sean Pristas, Architect. All design information contained in these drawings are for use only on this specified project and shall not be used otherwise without the expressed written permission by Sean Pristas, Architect.

PROJECT: 20180806.4

CLIENT: PALM DESERT, LLC  
28 HARBORVIEW  
PALM DESERT, CA 92211  
PHONE: 760.922.1111

NEW CONDITIONAL USE PERMIT FOR:  
**F2 - PALM DESERT, LLC**  
(CANNABIS STOREFRONT RETAILER)  
39420 BERKELEY DRIVE  
PALM DESERT, CA 92211

TITLE:  
PHOTO SHEET

DATE: 04/2024 PG: 01/01/01

ARCHITECT'S APPROVAL:

THIS PROJECT IS NOT APPROVED FOR CONSTRUCTION UNLESS THE PLANNING AND DESIGN SERVICES PROVIDED BY THE ARCHITECT AND THE BUILDING AUTHORITY HAVING JURISDICTION.

SHEET:  
**A1.31**

PHOTO SHEET SCALE: 0' 10' 20' 1

**NOTICE OF PUBLIC HEARING**  
**RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**  
[www.rcaluc.org](http://www.rcaluc.org)

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893**. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The County of Riverside may hold hearings on this item and should be contacted on non-ALUC issues. For more information please contact Riverside County Planner Mr. Gabriel Villalobos at (951) 955-6184.

The proposed project application may be viewed at [www.rcaluc.org](http://www.rcaluc.org). Written comments may be submitted to the Riverside County ALUC by e-mail to [prull@rivco.org](mailto:prull@rivco.org) or by U.S. mail to Riverside County Administrative Center, 4080 Lemon Street, 14<sup>th</sup> Floor, Riverside, California 92501. Individuals with disabilities requiring reasonable modifications or accommodations, please telephone Barbara Santos at (951) 955-5132.

**PLACE OF HEARING:** Riverside County Administration Center  
4080 Lemon Street, 1<sup>st</sup> Floor Board Chambers  
Riverside California

**DATE OF HEARING:** August 13, 2020

**TIME OF HEARING:** 9:30 A.M.

Pursuant to Executive Order N-25-20, this meeting will be conducted by teleconference only. Information on how to participate in the hearing will be available on the ALUC website at [www.rcaluc.org](http://www.rcaluc.org)

**CASE DESCRIPTION:**

ZAP1081BD20 – F2 Palm Desert LLC (Representative: Alberto Marciano) – Related Case: County of Riverside Case No. CUP190031 (Conditional Use Permit). The applicant proposes to establish a cannabis retail dispensary facility within an existing 2,572 square foot building on 0.94 acres located easterly of Berkey Drive, westerly of Washington Street, and northerly of Varner Road (Airport Compatibility Zone C of the Bermuda Dunes Airport Influence Area).



# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

## APPLICATION FOR MAJOR LAND USE ACTION REVIEW

BD  
Zone C

ALUC CASE NUMBER: ZAP 1081 BD 20      DATE SUBMITTED: June 10, 2020

**APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION**

<b>Applicant</b>	F2 - Palm Desert LLC	Phone Number (714) 522-5553
<b>Mailing Address</b>	2400 E. Katella Avenue, #655 Anaheim, CA 92806	Email (714) 522-5553
<b>Representative</b>	Alberto Marciano, Member and Manager	Phone Number (714) 522-5553
<b>Mailing Address</b>	<u>2400 E. Katella Avenue #655</u> <u>Anaheim, CA 92806</u>	Email <u>jon@czechandhowell.com</u>
<b>Property Owner</b>	Wishon Fresno LLC	Phone Number (714) 522-5553
<b>Mailing Address</b>	2400 E. Katella Avenue, #655 Anaheim, CA 92806	Email <u>jon@czechandhowell.com</u>

**LOCAL JURISDICTION AGENCY**

<b>Local Agency Name</b>	County of Riverside Planning Department	Phone Number 951-955-6184
<b>Staff Contact</b>	Gabriel Villalobos	Email <u>gvillalo@rivco.org</u>
<b>Mailing Address</b>	4080 Lemon Street, 12th Floor P.O. Box 1409, Riverside, California 92502-1409	Case Type
<b>Local Agency Project No</b>	CUP 190031	<input checked="" type="checkbox"/> General Plan / Specific Plan Amendment <input type="checkbox"/> Zoning Ordinance Amendment <input type="checkbox"/> Subdivision Parcel Map / Tentative Tract <input type="checkbox"/> Use Permit <input type="checkbox"/> Site Plan Review/Plot Plan <input type="checkbox"/> Other

**PROJECT LOCATION**

*Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways*

<b>Street Address</b>	39420 Berkey Drive, Palm Desert, CA 92211		
<b>Assessor's Parcel No.</b>	748-370-011 ; 748-370-012	<b>Gross Parcel Size</b>	0.94 ACREAGE
<b>Subdivision Name</b>		<b>Nearest Airport and distance from Airport</b>	Bermuda Dunes Airport 1.4 miles
<b>Lot Number</b>			

**PROJECT DESCRIPTION**

*if applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed*

<b>Existing Land Use (describe)</b>	Vacant Retail Location. The trees and signs are less than the required 45' regulations and are no taller than nearby trees. Please see attached site plans.



Proposed Land Use (describe)	The plan is to renovate 39420 Berkey Drive, Palm Desert, CA 92211, to establish a cannabis storefront retailer with delivery		
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)		
For Other Land Uses (See Appendix C)	Hours of Operation	11:00 AM TO 10:00 PM.	
	Number of People on Site	8	Maximum Number 70
	Method of Calculation	Total square feet of building divided 2,572 sq feet by 36.	
Height Data	Site Elevation (above mean sea level)	220	ft.
	Height of buildings or structures (from the ground)	19' 6"	ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?		<input type="checkbox"/> Yes
	if yes, describe		<input checked="" type="checkbox"/> No

- A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:**
1. . . . . Completed ALUC Application Form
  1. . . . . ALUC fee payment
  1. . . . . Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
  1. . . . . Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
  1. . . . . CD with digital files of the plans (pdf)
  1. . . . . Vicinity Map (8.5x11)
  1. . . . . Detailed project description
  1. . . . . Local jurisdiction project transmittal
  3. . . . . Gummed address labels for applicant/representative/property owner/local jurisdiction planner
  3. . . . . Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. (Only required if the project is scheduled for a public hearing Commission meeting)

**COUNTY OF RIVERSIDE  
AIRPORT LAND USE COMMISSION**

**STAFF REPORT**

**AGENDA ITEM:** 3.5

**HEARING DATE:** August 13, 2020

**CASE NUMBER:** ZAP1086PS20 – Desert Aids Project (Representative: Terra Nova Planning & Research)

**APPROVING JURISDICTION:** City of Palm Springs

**JURISDICTION CASE NO:** 5.0934 (PD 281 Amendment, General Plan Amendment), 3.1047 (Major Architectural)

**LAND USE PLAN:** 2005 Palm Springs International Airport Land Use Compatibility Plan

**Airport Influence Area:** Palm Springs International Airport

**Land Use Policy:** Compatibility Zones C, E

**Noise Levels:** Below 60 CNEL from aircraft noise

**MAJOR ISSUES:** The project proposes residential density, non-residential intensity (both average and single acre), and infiltration basins (wildlife hazards) that are all incompatible with Compatibility Zone C criteria. The apartment complex proposes 61 units on 1.14 acres, resulting in an overall density of 54 dwelling units per acre, which is inconsistent with Zone C Additional Compatibility Policy #2.2 residential density criteria. The Pavillon proposes an 18,500 square foot building, accommodating 588 people, resulting in an average intensity of 151 people per acre, and a single acre intensity of 588 people, both of which are inconsistent with Zone C intensity criteria. The proposed 17,700 square foot infiltration basin-dog park, specifically the use of landscaping on its side slopes, the basin drawdown time of 72 hours, and the non-qualification of the study preparer are all inconsistent with the goals and policies of the ALUC wildlife hazard study and FAA circular.

**RECOMMENDATION:** Staff recommends that the Commission find the proposed General Plan Amendment and Planned Development District Amendment INCONSISTENT with the 2005 Palm Springs International Airport Land Use Compatibility Plan, and find the proposed Major Architectural INCONSISTENT, based on the fact that the project's proposed residential density, non-residential average and single acre intensity, and detention basins are inconsistent with the Compatibility Zone C criteria.

**PROJECT DESCRIPTION:** The applicant proposes to expand the existing Desert AIDS Project Campus by constructing a new 18,500 square foot medical ‘Pavillon’ building which includes a lobby, information desk, registration, nurse station, medical administration rooms, conference meeting rooms, consultation offices, and café on 3.89 acres, a 61 unit affordable housing apartment complex on 1.14 acres, and a 17,700 square foot dog park-infiltration basin on 3.09 acres, for a total of 8.12 acres (of a larger existing 13.02 acre site). The applicant also proposes amending the General Plan land use designation of the site from Medium Density Residential and Public/Quasi Public to Mixed Use/Multi-Use. The applicant also proposes to amend the Planned Development District 281, by expanding the project boundary to include all aspects of the project and to allow implementation of the Mixed Use land use designation

**PROJECT LOCATION:** The site is located on the southwest corner of Vista Chino and Sunrise Way, within the City of Palm Springs, approximately 3,140 feet northwesterly of the northerly end of Runway 13L-31R at Palm Springs International Airport.

**BACKGROUND:**

Residential Density: Pursuant to the Palm Springs International Airport Land Use Compatibility Plan, the proposed apartment complex is entirely located within Compatibility Zone C, where Additional Compatibility Policy #2.2 restricts residential densities to either a maximum 0.2 dwelling units per acre, or between 3.0 to 15.0 dwelling units per acre.

The project proposes 61 units on 1.14 acres, resulting in an overall density of 54 dwelling units per acre, which is inconsistent with Zone C Additional Compatibility Policy #2.2 residential density criteria to either a maximum 0.2 dwelling units per acre, or between 3.0 to 15.0 dwelling units per acre.

Policy #3.1.3.d in the Riverside County Airport Land Use Compatibility Plan allows for mixed use projects to calculate its residential component (which would have normally been calculated as density) as non-residential intensity. The project proposes 42,609 square feet of apartment building area, which includes 29,405 square feet of apartment unit area (147 people), and community facilities which includes 365 square feet of manager office area (2 people), 110 square feet of lobby area (7 people), 555 square feet of multi-purpose room area (37 people), 255 square feet of maintenance area (1 person), and 270 square feet of laundry room area (1 person), accommodating an occupancy of 195 people, resulting in an average intensity of 171 people per acre, and a single acre intensity of 195 people, both of which are inconsistent with Compatibility Zone C average intensity criteria of 75 people per acre, and single acre intensity criteria of 150 people in any given single acre.

County Wide Policy 3.3.1 Infill: Countywide Policy 3.3.1 (Infill) allows for greater densities than would otherwise be permitted in Compatibility Zone C, but caps densities at double the allowable density of the zone. As the maximum density of the zone is 15 dwelling units per acre (Additional Compatibility Policy #2.2), doubling the density increases the limit from 15 to 30 dwelling units per acre, which would still not be close to the project’s density of 54 dwelling units per acre.

Non-Residential Average Intensity: Pursuant to the Airport Land Use Compatibility Plan for Palm Springs International Airport, the proposed project development scope is located within Compatibility Zones C (7.02 acres) and E (1.10 acres). Non-residential intensity in Compatibility Zone C is restricted to an average intensity of 75 people per acre (Zone E does not restrict intensity). The proposed Pavillon building is located entirely within the Zone C portion of the property (3.89 acres). The dog park-infiltration basin is located in Zones C (0.6 acre) and E (2.5 acres) and does not propose any buildings.

The Building Code Method for calculating intensity utilizes “minimum floor area per occupant” criteria from the Building Code as a factor in projecting intensity. Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, the following intensities were utilized for the project:

- nurse, office, administration, consultation – 1 person per 200 square feet, and
- conference room, lobby area, cafe – 1 person per 15 square feet.

The proposed 18,500 square foot Pavillon building includes 475 square foot consultation area, 3,182 square foot lobby area, 210 square foot nurses area, 290 square foot information area, 535 square foot registration area, 1,115 square foot medical administration area, 4,145 square foot meeting conference room area, 610 square foot consulting offices, 1,100 square foot café restaurant area, and 2,010 square foot office area, accommodating 588 people, resulting in an average intensity of 151 people per acre, which is inconsistent with the Compatibility Zone C average acre criterion of 75.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per standard vehicle). Based on the number of provided parking spaces for the apartments (31 spaces) and Pavillon (261 shared spaces), the total occupancy would be estimated at 438 people, resulting in an average intensity of 87 people per acre (based on the 3.89 acre Pavillon parcel and 1.14 acre apartment parcel), which is inconsistent with the Compatibility Zone C average acre criterion of 75.

If we considered the occupancies at the existing 39,923 square foot DAP building and 22,000 square foot DAP annex building, all located on the same 3.89 acre parcel as the proposed Pavillon building, the total occupancy would accommodate 928 people, resulting in an average intensity of 239 people per acre, which is inconsistent with the Compatibility Zone C average acre criterion of 75.

Non-Residential Single-Acre Intensity: As previously noted, the proposed Pavillon building is located within Compatibility Zone C, which restricts non-residential intensity to 150 people in any given single acre.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre intensity occurs around the Pavillon building, and includes 475 square foot consultation area, 3,182 square foot lobby area, 210 square foot nurses area, 290 square foot information area, 535 square

foot registration area, 1,115 square foot medical administration area, 4,145 square foot meeting conference room area, 610 square foot consulting offices, and 1,100 square foot café restaurant area, accommodating a total occupancy of 588 people, which is inconsistent with the Compatibility Zone C single acre intensity criterion of 150 people.

If we considered the occupancies at the existing 39,923 square foot DAP building and 22,000 square foot DAP annex building, and factored them into the calculation, the maximum single-acre intensity would include the entire 18,500 square foot Pavillon building (588 people), 10,200 square feet of medical rooms in the DAP building (51 people), and 7,000 square feet of medical rooms in the DAP annex building (35 people), accommodating a total occupancy of 674 people, which is inconsistent with the Compatibility Zone C single acre intensity criterion of 150 people.

Prohibited and Discouraged Uses: The applicant does not propose any new use specifically prohibited or discouraged in Compatibility Zones C (children's schools, day care centers, libraries, hospitals, nursing homes, buildings with 3 aboveground habitable floors, highly noise-sensitive outdoor nonresidential uses) and E of the Palm Springs International Airport Influence Area. (The proposed Pavillon does not contain any critical medical uses within its footprint, as it contains more ancillary uses servicing the existing medical clinics.) However, the project does propose detention basins that could be a hazard to flight, apartment density, and Pavillon non-residential intensity that are inconsistent with Zone C criteria.

Noise: The Palm Springs Airport Land Use Compatibility Plan depicts the site as being in an area below 60 CNEL from aircraft. Therefore, special measures to mitigate aircraft-generated noise would not be required.

Part 77: The elevation of Runway 13L-31R at its northerly terminus is 474.4 feet above mean sea level (AMSL). At a distance of approximately 3,138 feet from the runway to the project, Federal Aviation Administration (FAA) review would be required for any structures with a top point elevation exceeding 505.8 feet AMSL. The site's elevation is 509 feet AMSL and the proposed tallest structure height (apartments) is 34 feet, for a top point elevation of 543 feet AMSL. Therefore, review by the FAA Obstruction Evaluation Service for height/elevation reasons was required. Submittal to the FAAOES was made and Aeronautical Study Numbers 2020-AWP-6798-OE and 2020-AWP-6815-OE were assigned to the project. Determination of No Hazard to Air Navigation letters were issued on July 1, 2020. The FAA OES determined that the project would not result in an impact to air navigation, provided that the project complies with the conditions in that letter (which have been included in staff's recommended conditions).

Open Area: Compatibility Zone C requires 20% of the land area within major projects (10 acres or larger) be set aside as open area that could potentially serve as emergency landing areas (Zone E does not require any open area). Since the proposed new development is less than 10 acres (8.12 acres), the open area requirement is not applicable to this project.

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33C). The project is located 3,140 feet from the runway, and therefore would be subject to the above requirement.

The project includes approximately 17,700 square feet of infiltration basin area to be incorporated within the dog park. Infiltration basin areas are not recommended in the vicinity of airports due to the potential that such areas could provide food, water, and shelter for hazardous wildlife. Pursuant to the study “Wildlife Hazard Management at Riverside County Airports: Background and Policy”, October 2018, by Mead & Hunt, which is the basis of the brochure titled “Airports, Wildlife and Stormwater Management”, such basins are potentially suitable within 10,000 feet of the airport only with appropriate modifications such as: 48 hours drawdown, steep slopes (steeper than 3:1), avoid landscaping or provide appropriate landscaping reviewed by a qualified biologist that will not be attractive to hazardous wildlife and adequately maintained, and consider the use of covers.

In order to evaluate this potential impact, a qualified FAA wildlife hazard biologist is required. The applicant has chosen not to commission a wildlife hazard study from a qualified FAA wildlife hazard biologist. Instead, a (non-qualified) wildlife hazard report was prepared by the applicant’s team, Terra Nova Planning & Research, dated June 18, 2020. The report indicates that the proposed basin will contain landscaping on its side slopes and decomposed granite on the bottom, and will be required to drain within 72 hours of any storm event. The report also gives testimony that “no incident of flocks of birds” or “bird strikes” have been identified at the site for the past 11 years. It concludes that the propose basin will not result in any hazard to the Palm Springs International Airport from increased bird activity.

The design of the proposed basin, specifically, the use of landscaping on its side slopes, the basin drawdown time of 72 hours, and the non-qualification of the study preparer are all inconsistent with the goals and policies of the ALUC wildlife hazard study and FAA circular.

General Plan Amendment/Rezone: The proposed General Plan Amendment (changing land use designation from Medium Density Residential and Public/Quasi Public to Mixed Use/Multi-Use) and Planned Development District 281 amendment (expanding the project boundary to include all aspects of the project and to allow implementation of the Mixed Use land use designation) would allow for the proposed development.

However, given the location of the site within Compatibility Zone C, the apartment’s proposed density of 54 dwelling units per acre, the Pavillon’s average intensity of 151 people per acre and single acre intensity of 588 people, and the dog park-infiltration basin’s potential wildlife hazard, are

all incompatibility with the Compatibility Zone C criteria, and therefore prohibited at this location. Therefore, the proposed General Plan Amendment and Planned Development District 281 amendment are inconsistent with the 2005 Palm Springs International Airport Land Use Compatibility Plan.

**CONDITIONS:**

1. Any new outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, noise sensitive outdoor nonresidential uses and hazards to flight.
3. The attached notice shall be given to all prospective purchasers, lessees, and/or tenants of the property, and shall be recorded as a deed notice.
4. Any proposed detention basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry

between rainfalls. Vegetation in and around the detention basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the detention basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at [RCALUC.ORG](http://RCALUC.ORG) which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

5. The ALUC Presence of Aircraft Overflight informational brochure shall be provided to prospective renters showing the locations of aircraft flight patterns, frequency of overflights, typical altitudes of the aircraft, and the range of noise levels that can be expected from individual aircraft overflights.
6. An informational sign shall be posted in the leasing/rental office clearly depicting the proximity of the project to Palm Springs International Airport and aircraft traffic patterns as identified in the ALUC Presence of Aircraft Overflight informational brochure.
7. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission.
8. The Federal Aviation Administration has conducted aeronautical studies of the proposed project (Aeronautical Study Nos. 2020-AWP-6798-OE and 2020-AWP-6815-OE) and has determined that neither marking nor lighting of the buildings are necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 L Change 2 and shall be maintained in accordance therewith for the life of the project.



9. The proposed building shall not exceed a height of 34 feet above ground level and a maximum elevation at top point of 543 feet above mean sea level.
10. The maximum height and top point elevation specified above shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in structure height or elevation shall not require further review by the Airport Land Use Commission.
11. Temporary construction equipment used during actual construction of the structure(s) shall not exceed 34 feet in height and a maximum elevation of 543 feet above mean sea level, unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.
12. Within five (5) days after construction of the proposed building reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned or a decision is made not to construct the applicable structure.

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

# NOTICE

**THERE IS AN AIRPORT NEARBY.  
THIS STORM WATER BASIN IS DESIGNED TO HOLD  
STORM WATER FOR ONLY 48 HOURS AND  
NOT TO ATTRACT BIRDS**

**PROPER MAINTENANCE IS NECESSARY TO AVOID  
BIRD STRIKES**



**IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:**

**Name:** \_\_\_\_\_

**Phone:** \_\_\_\_\_



Mail Processing Center  
 Federal Aviation Administration  
 Southwest Regional Office  
 Obstruction Evaluation Group  
 10101 Hillwood Parkway  
 Fort Worth, TX 76177

Aeronautical Study No.  
 2020-AWP-6798-OE

Issued Date: 07/01/2020

David Brinkman  
 Desert AIDS Project  
 1695 North Sunrise Way  
 Palm Springs, CA 92262

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building DAP Expansion Project  
 Location: Palm Springs, CA  
 Latitude: 33-50-34.44N NAD 83  
 Longitude: 116-31-43.59W  
 Heights: 509 feet site elevation (SE)  
 34 feet above ground level (AGL)  
 543 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/01/2022 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within

6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or [vivian.vilaro@faa.gov](mailto:vivian.vilaro@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-AWP-6798-OE.

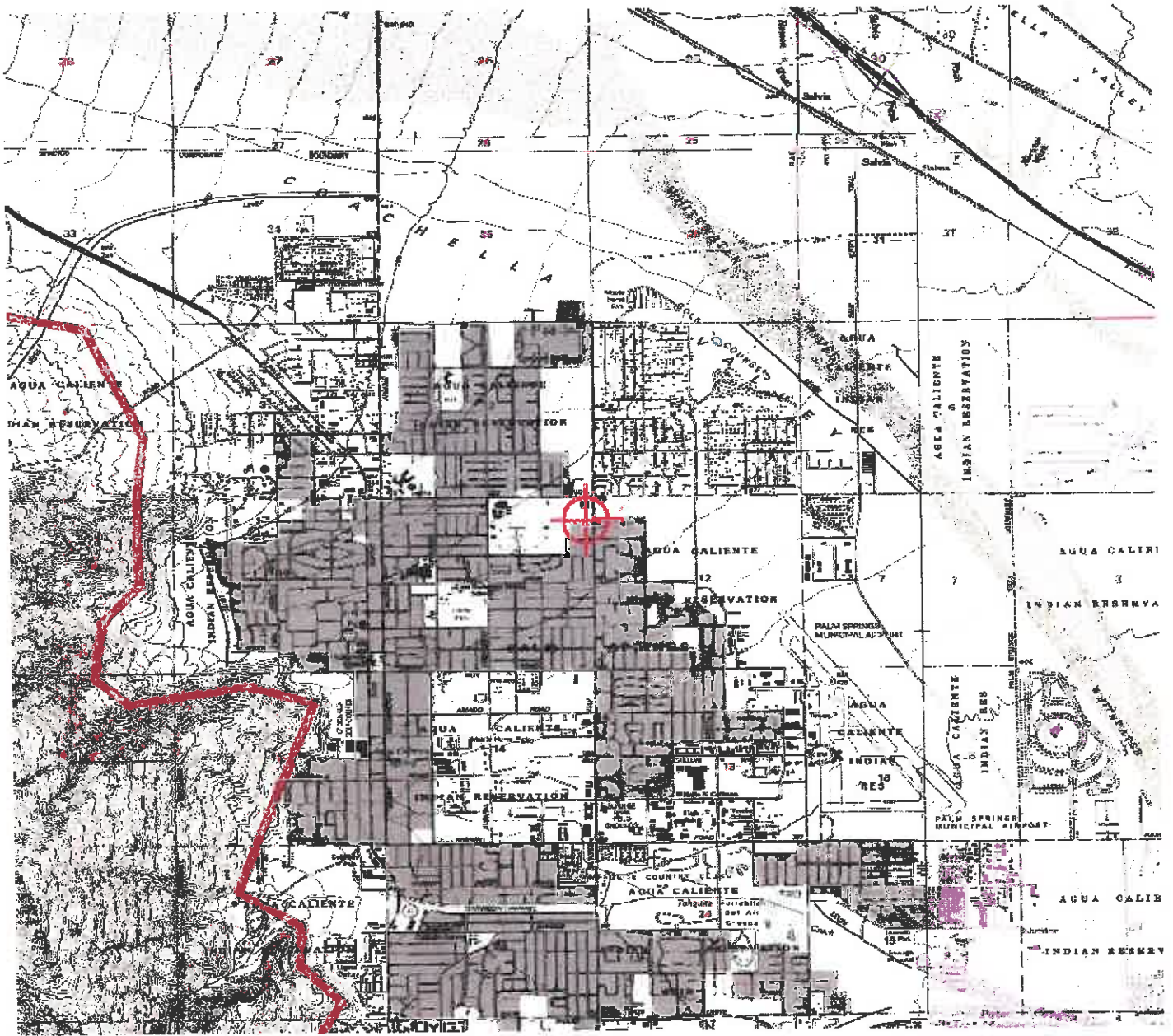
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( DNE )

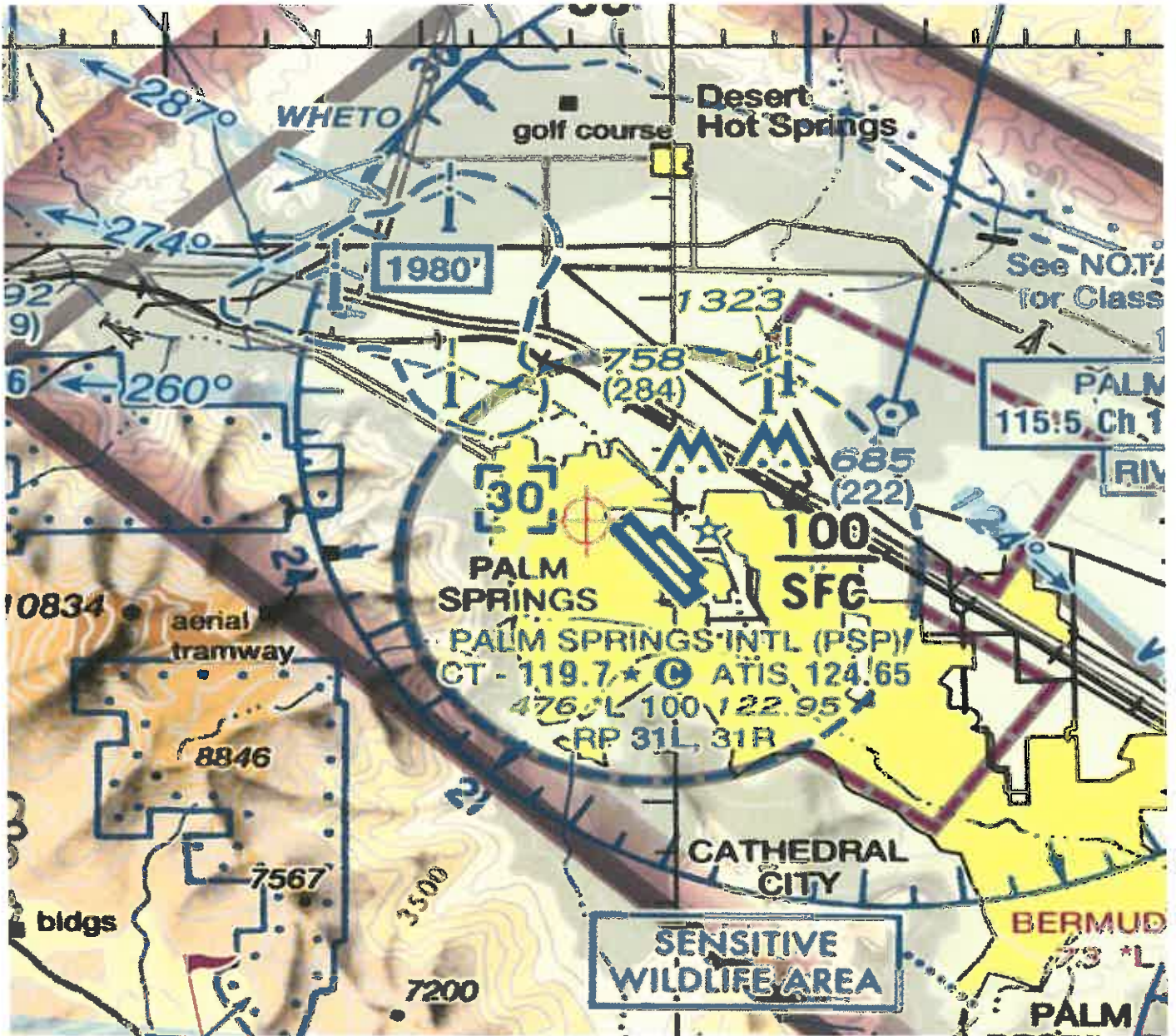
Vivian Vilaro  
Specialist

Attachment(s)  
Map(s)

TOPO Map for ASN 2020-AWP-6798-OE



Sectional Map for ASN 2020-AWP-6798-OE





Mail Processing Center  
 Federal Aviation Administration  
 Southwest Regional Office  
 Obstruction Evaluation Group  
 10101 Hillwood Parkway  
 Fort Worth, TX 76177

Aeronautical Study No.  
 2020-AWP-6815-OE

Issued Date: 07/01/2020

David Brinkman  
 Desert AIDS Project  
 1695 North Sunrise Way  
 Palm Springs, CA 92262

**\*\* DETERMINATION OF NO HAZARD TO AIR NAVIGATION \*\***

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building DAP Pavilion  
 Location: Palm Springs, CA  
 Latitude: 33-50-37.12N NAD 83  
 Longitude: 116-31-44.57W  
 Heights: 512 feet site elevation (SE)  
 24 feet above ground level (AGL)  
 536 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 01/01/2022 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within



6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

**NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.**

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (847) 294-7575, or [vivian.vilaro@faa.gov](mailto:vivian.vilaro@faa.gov). On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-AWP-6815-OE.

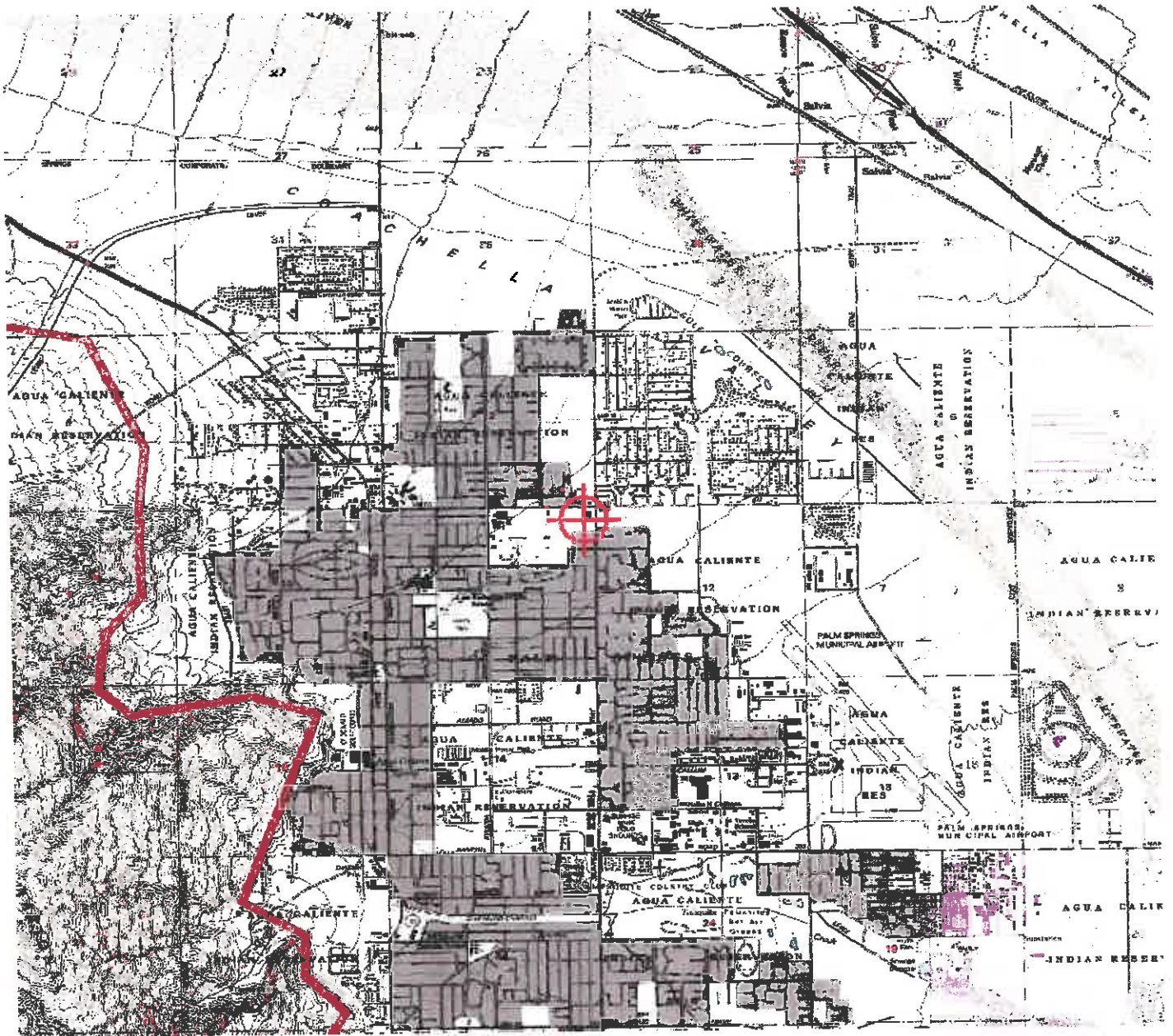
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( DNE )

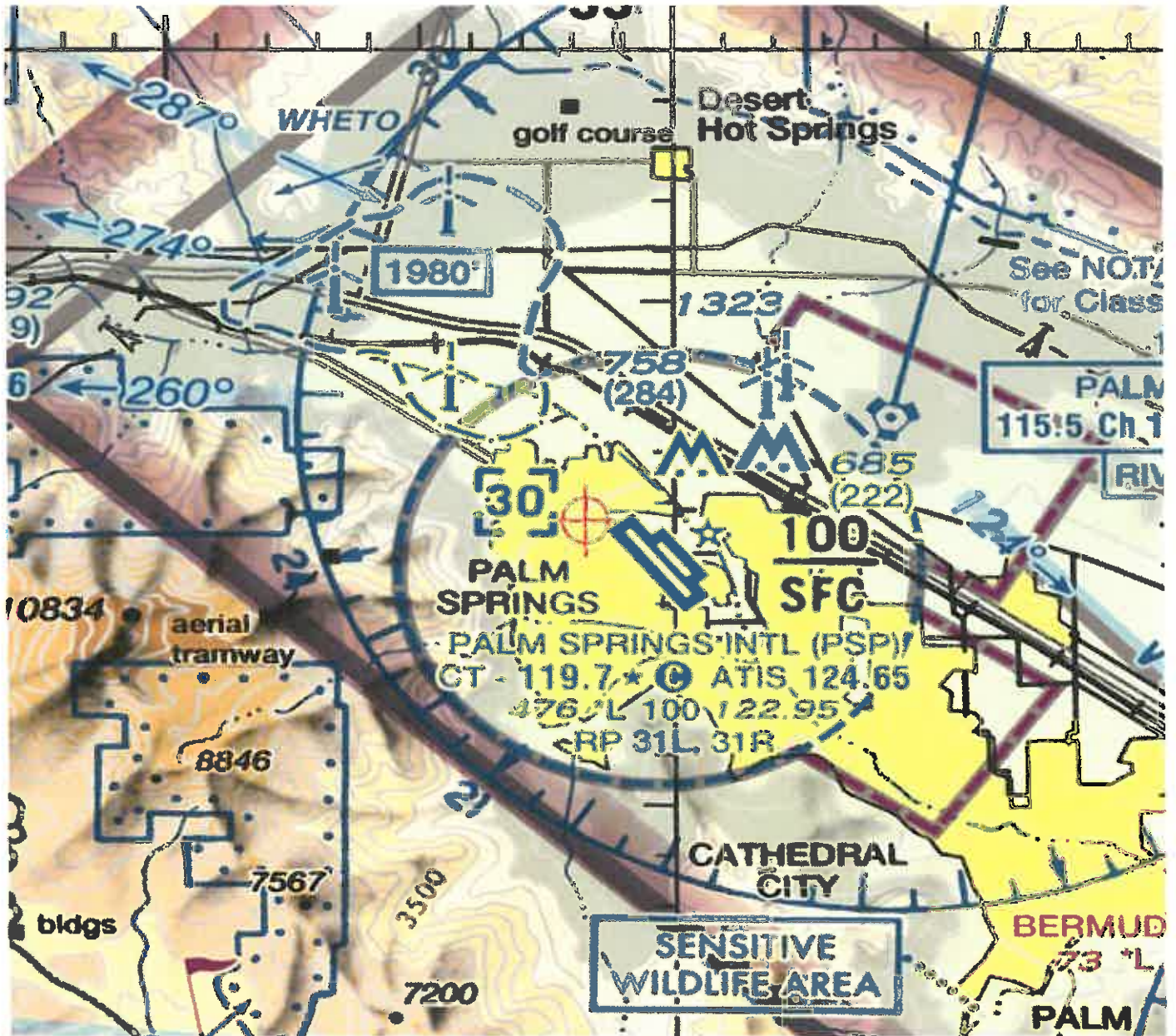
Vivian Vilaro  
Specialist

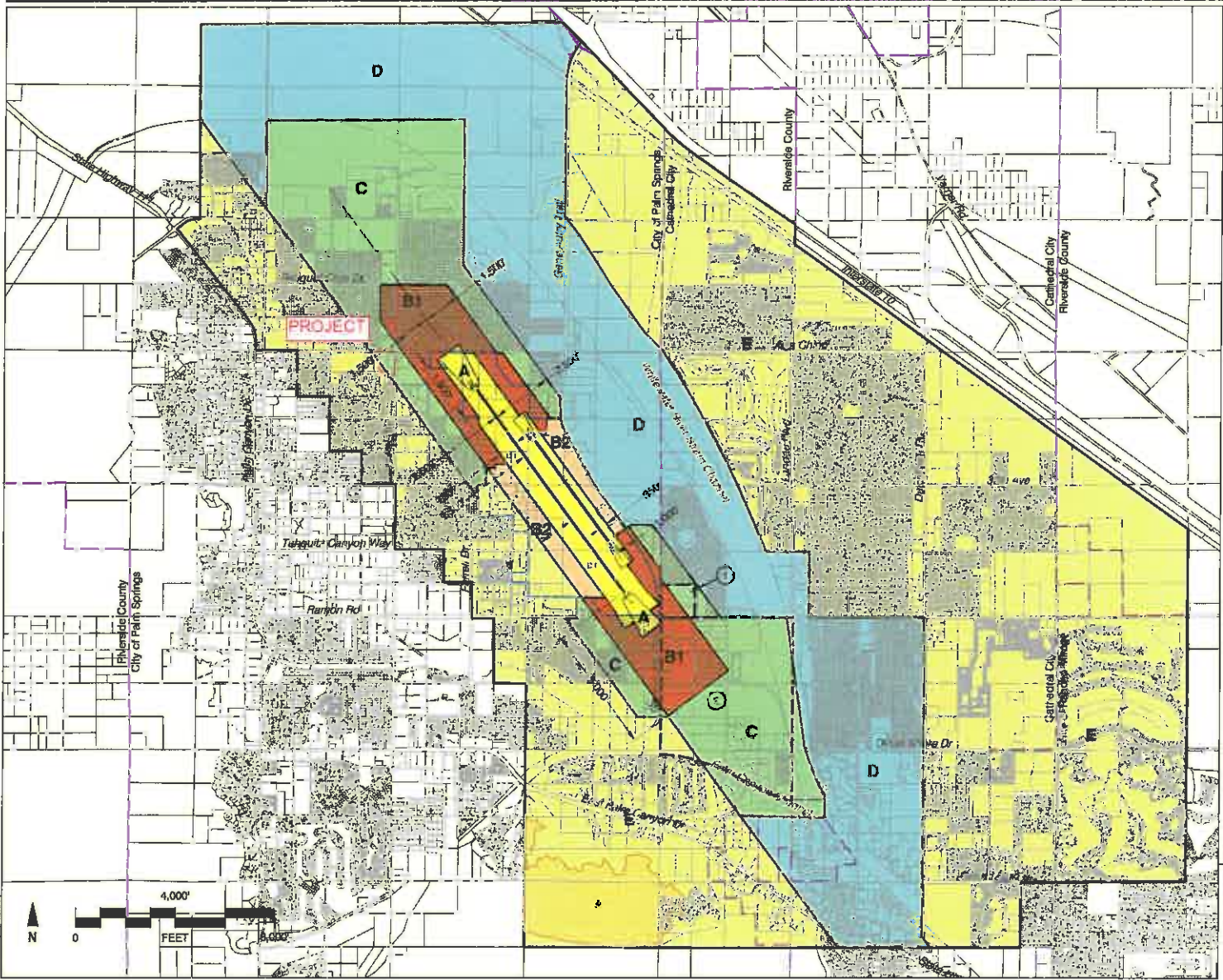
Attachment(s)  
Map(s)

TOPO Map for ASN 2020-AWP-6815-OE



Sectional Map for ASN 2020-AWP-6815-OE





**Legend**

- Compatibility Zones**
- Airport Influence Area Boundary
  - Zone A
  - Zone B1
  - Zone B2
  - Zone C
  - Zone D
  - Zone E
  - Height Review Overlay Zone

- Boundary Lines**
- Airport Property Line
  - City Limits

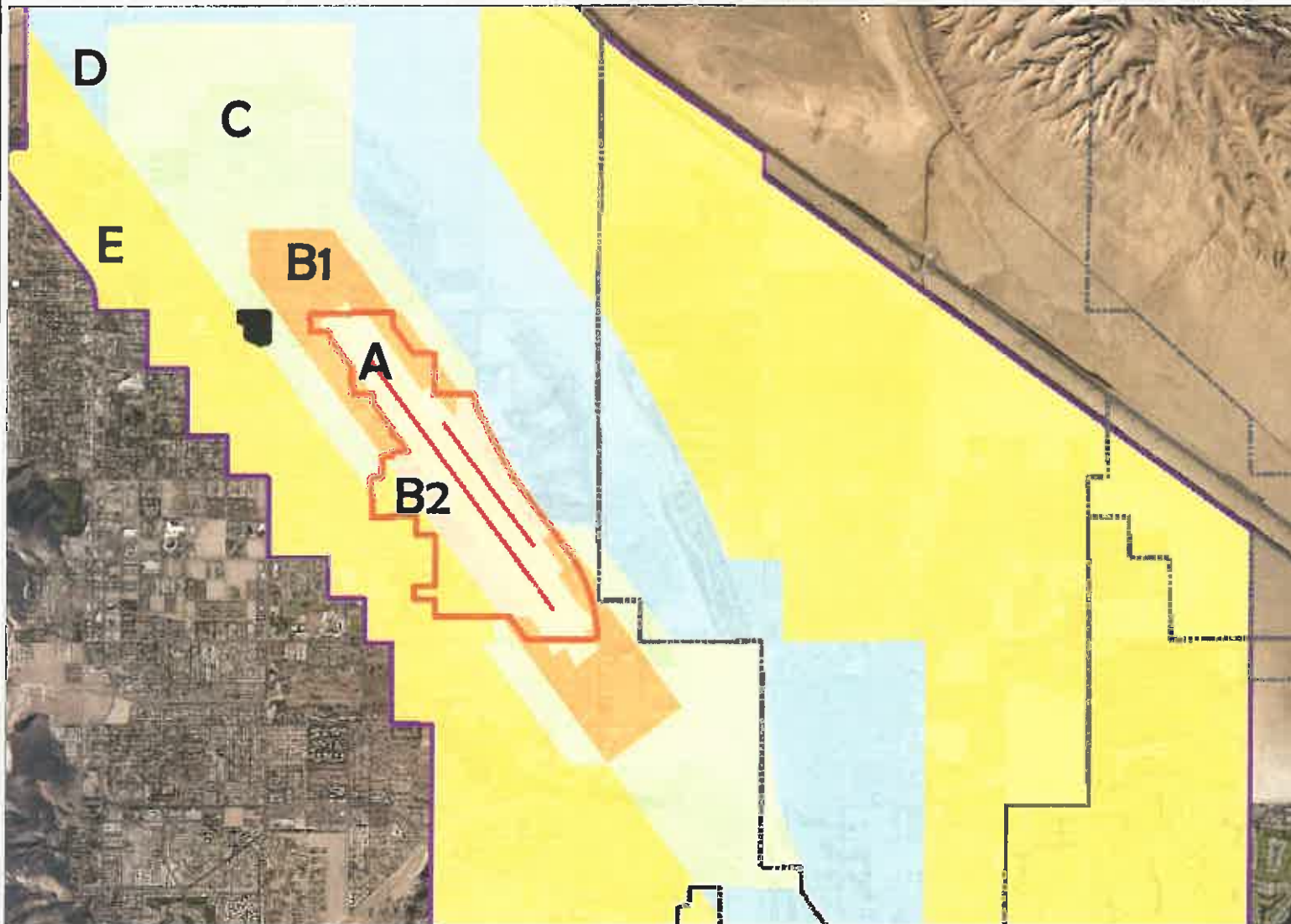
- Notes**
- All dimensions measured from runway ends and centerlines.
  - DT = Displaced Threshold
  - See Chapter 2, Table 2A for compatibility criteria associated with this map.
  - See Policy PS.2.1.

Riverside County  
 Airport Land Use Commission  
 Riverside County  
 Airport Land Use Compatibility Plan  
 Policy Document  
 (Adopted March 2005)

Map PS-1

**Compatibility Map**  
 Palm Springs International Airport

# Map My County Map



### Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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### Notes

# Map My County Map



## Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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0 376 752 Feet

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## Notes

# Map My County Map



## Legend

- Parcels
- World Street Map



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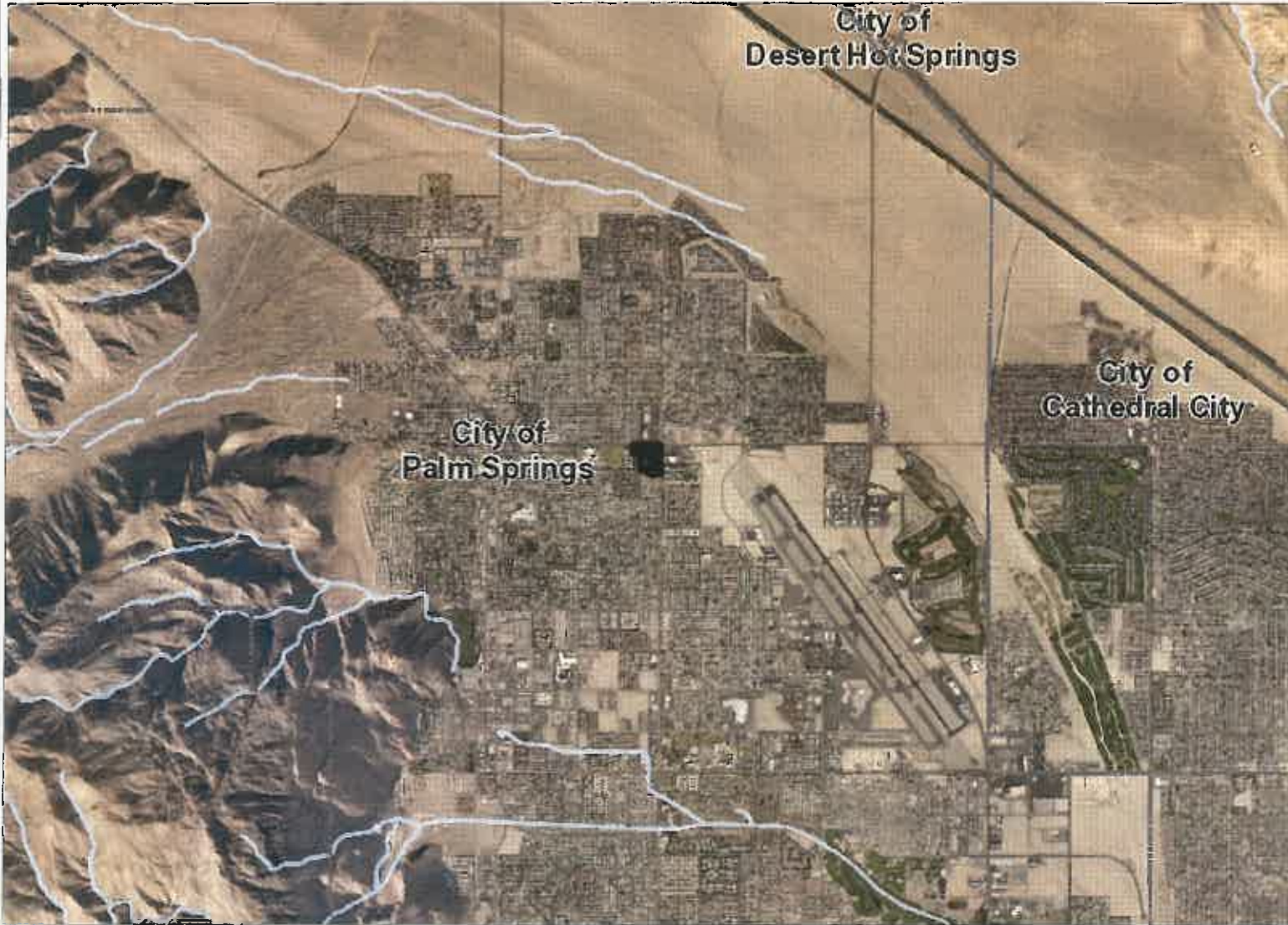
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## Notes

# Map My County Map



## Legend

- Blueline Streams
- City Areas
- World Street Map



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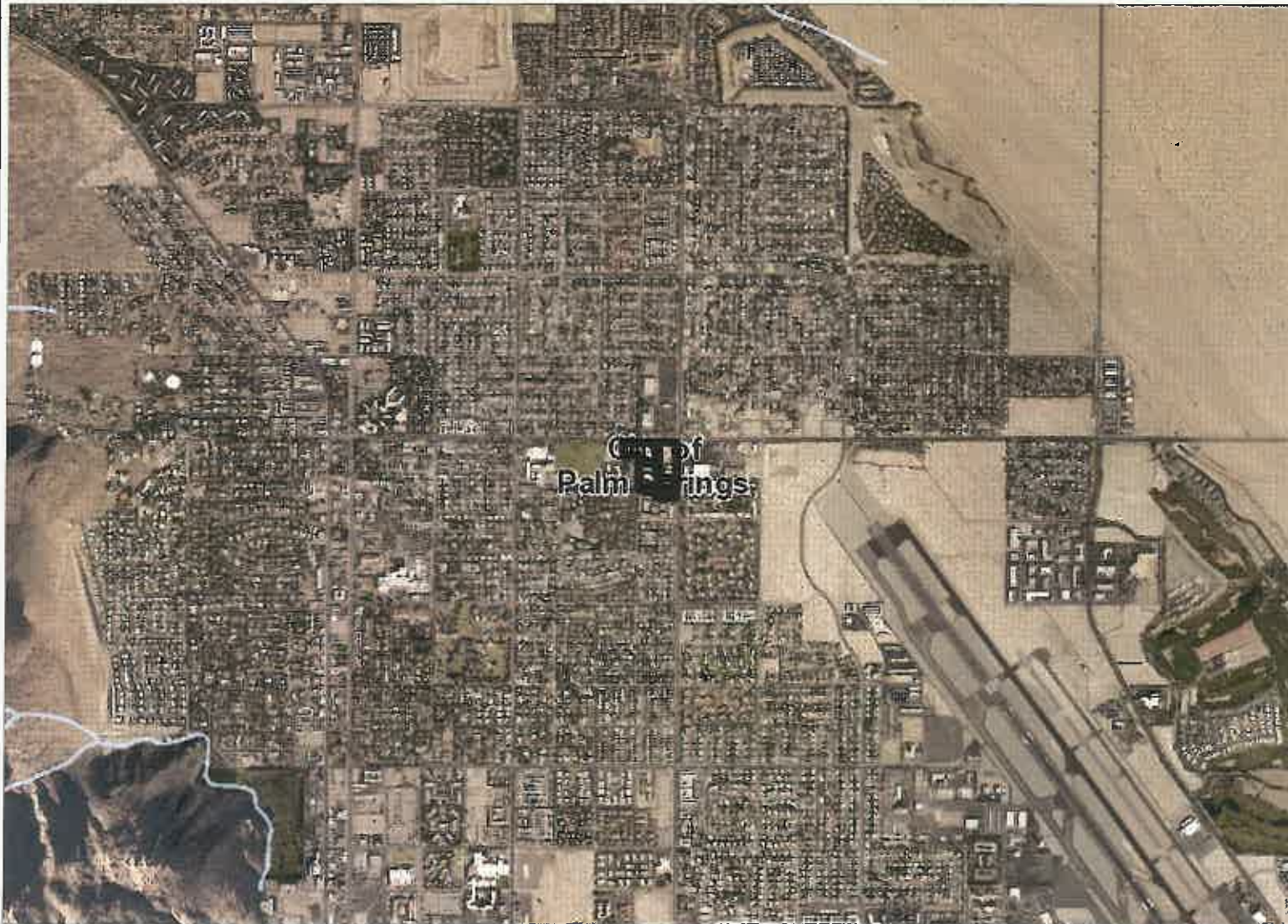
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## Notes



# Map My County Map



## Legend

- Blueline Streams
- City Areas
- World Street Map



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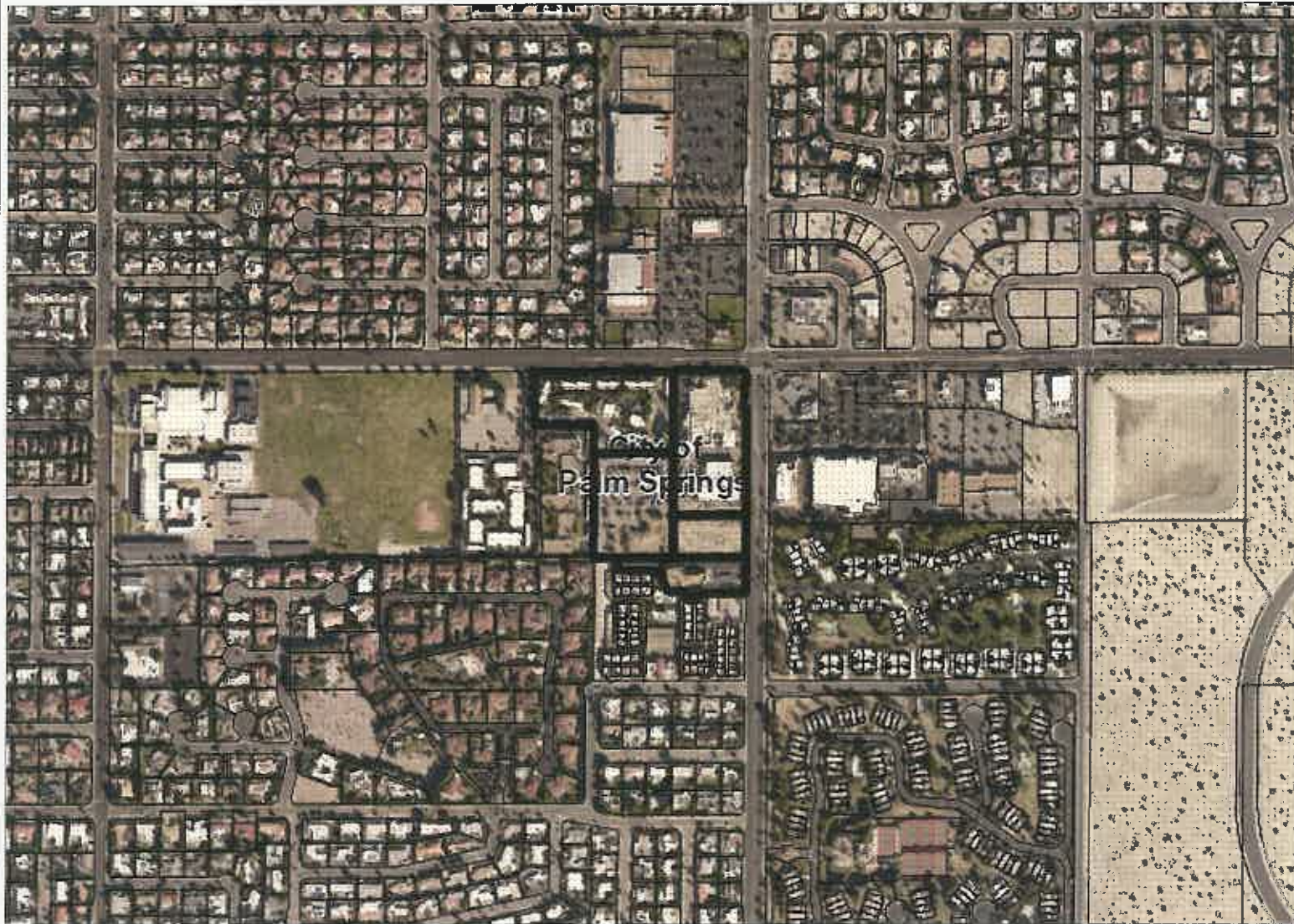
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## Notes

# Map My County Map



## Legend

-  Parcels
-  Blueline Streams
-  City Areas
-  World Street Map



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0 752 1,505 Feet

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## Notes

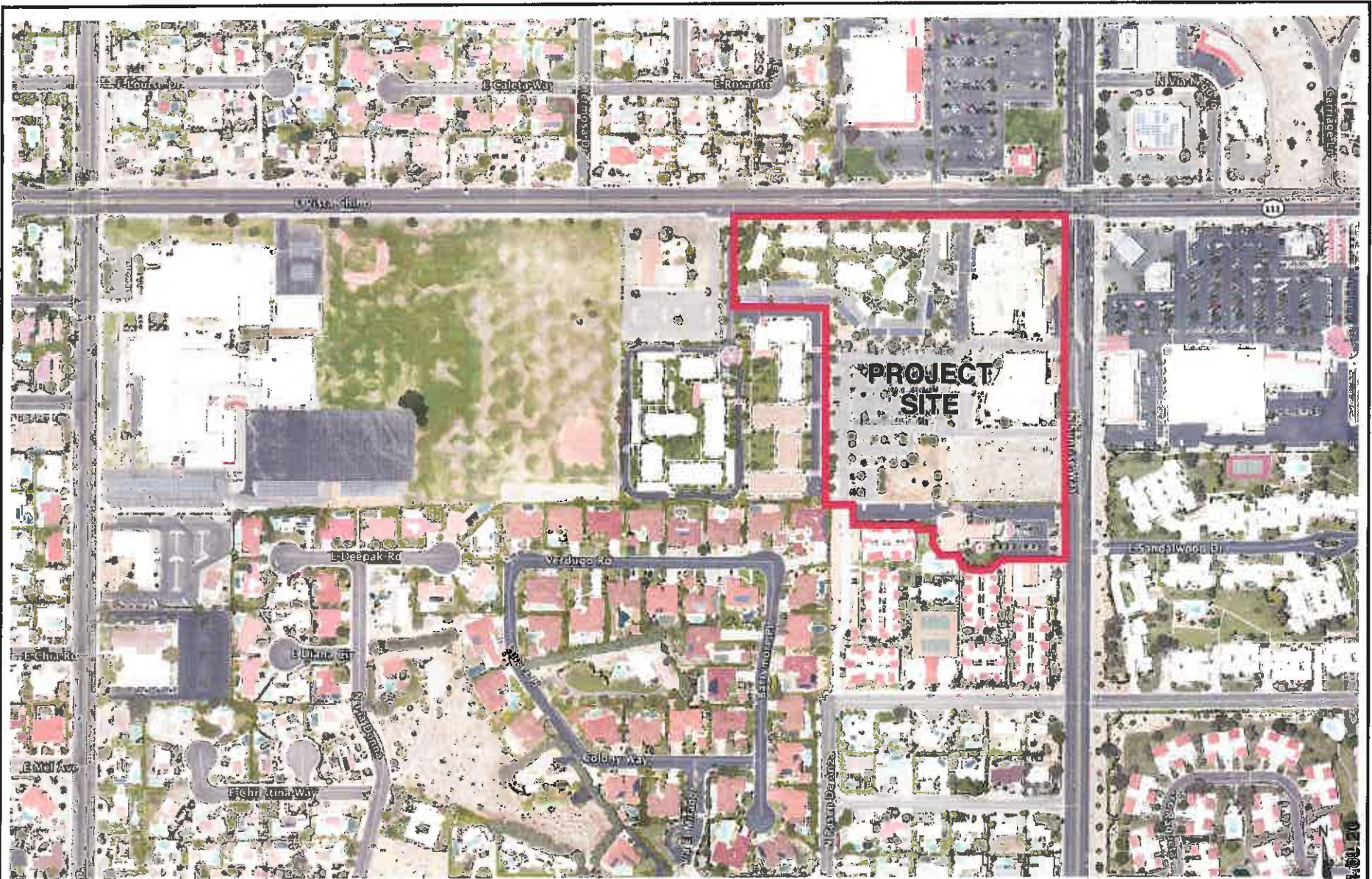
## **Desert AIDS Project Campus Expansion Project Description**

Desert AIDS Project (DAP), in partnership with the Coachella Valley Housing Coalition (CVHC), proposes an expansion of the current DAP campus. The project includes an 18,500± square foot Pavilion which will be an addition to the medical services buildings, and comprehensive access, site and landscaping improvements on the existing DAP campus; and the construction of 61 special needs affordable housing units immediately south of the campus in partnership with CVHC.

The Pavilion will provide a new single point entrance to all clients of DAP. The new uses within the Pavilion will include a lobby, information desk, registration, triage nurse, computer lab, consultation, gallery, café, new food depot, new pharmacy and safe passage to medical, dental, behavioral health, social wellness, and physical wellness services. The site improvements will also include a façade update to the existing building, realignment of the existing driveways, parking enhancements and improvements, new monument signage and comprehensive landscape improvements across the entire campus.

Special needs housing will be provided for DAP clients in partnership with CVHC. In addition to the 60 very-low and low income household units and one manager's unit proposed, the site program includes counseling, on-site management, social gathering spaces, a community room, and common area courtyards. The apartments will be studio and one-bedroom units on three stories. The layout of the units strongly encourages socialization, and the use of natural light and mountain views for a healthy living environment.

The project area encompasses a total of 13.2 acres, and all of this land is proposed to be included within the PDD boundary, to assure a cohesive and unified project now and in the future. The land includes the recently acquired office building at 1495 North Sunrise. No alterations are being proposed for this site at this time, but the building will eventually be occupied by DAP administrative offices, and will be connected to the rest of the campus via an integrated walkway which extends along the entire north-south axis of the site, along the project's main roadway.



Source: Google Earth, 2020

### Existing Land Use Map



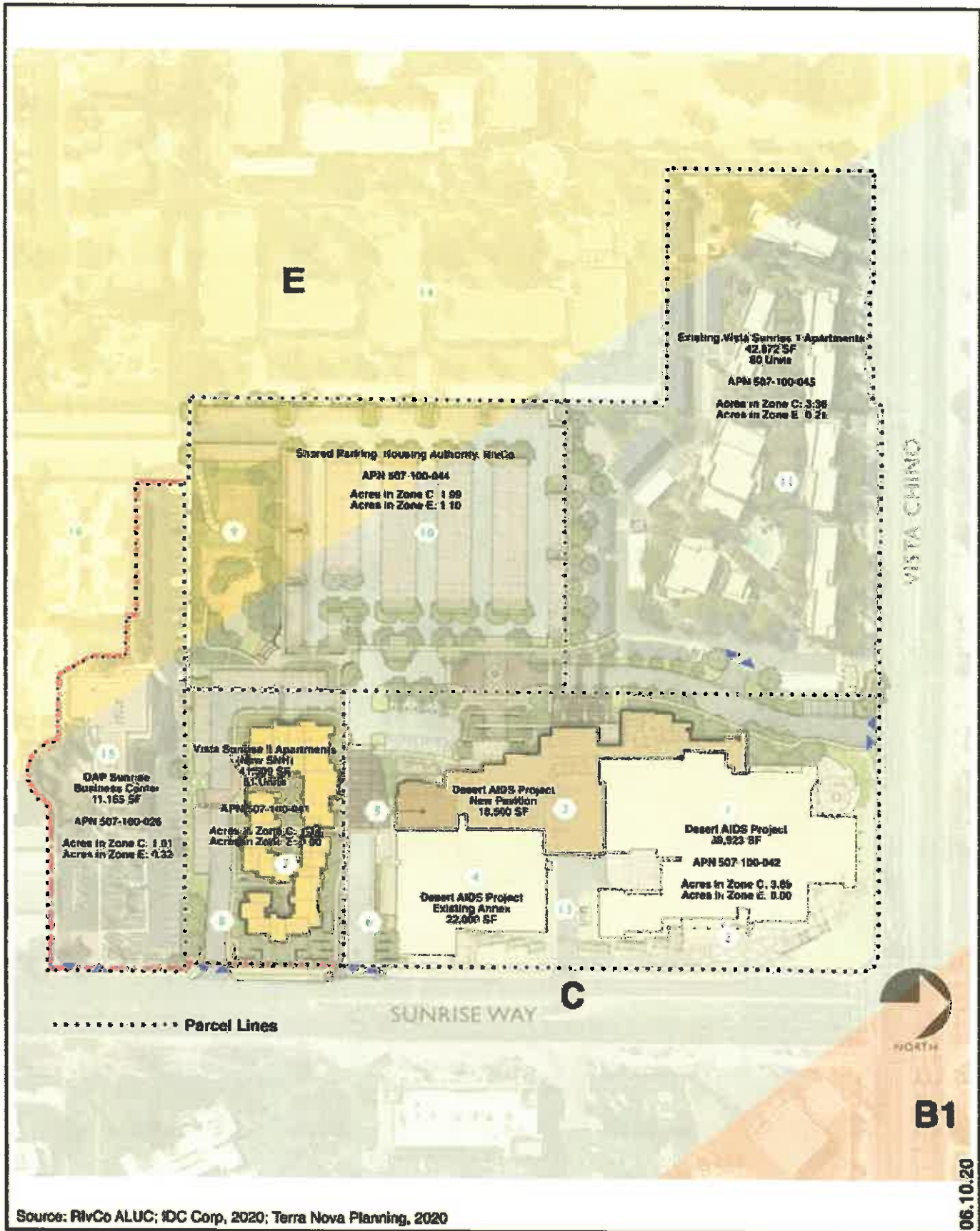
### Proposed Land Use Map



	Estate Residential (up to 2.0 du/ac)		Mixed Use/Multi-Use
	Very Low Density Residential (up to 4.0 du/ac)		Office
	Low Density Residential (up to 6.0 du/ac)		Industrial
	Medium Density Residential (up to 15.0 du/ac)		School
	Neighborhood/Community Commercial		Airport
	Open Space - Water		Public/Quasi-Public

Source: Palm Springs General Plan, 2014; Terra Nova Planning, 2020

N  
04.09.20



**CAMPUS AERIAL**

SCALE: 1" = 70'-0"



**LEGEND**

- 1 DESERT AIDS PROJECT  
APN 507-100-042
- 2 DESERT AIDS PROJECT  
EXISTING CAMPUS ENTRANCE
- 3 DESERT AIDS PROJECT  
NEW PAVILION
- 4 DESERT AIDS PROJECT  
EXISTING ANNEX
- 5 DESERT AIDS PROJECT  
NEW FOOD DEPOT
- 6 DESERT AIDS PROJECT  
WIDENED DRIVEWAY
- 7 VISTA SUNRISE II APARTMENTS (NEW SNH)  
APN 507-100-041
- 8 VISTA SUNRISE II APARTMENTS (NEW SNH)  
SITE ENTRY / EXIT
- 9 RECONFIGURED & IMPROVED RETENTION BASIN  
APN 507-100-044
- 10 SHARED PARKING, HOUSING AUTHORITY OF THE  
COUNTY OF RIVERSIDE - APN 507-100-044
- 11 EXISTING VISTA SUNRISE I APARTMENTS  
APN 507-100-045
- 12 DESERT AIDS PROJECT  
NEW REALIGNED DRIVEWAY & ARRIVAL COURT
- 13 DESERT AIDS PROJECT - SERVICE DRIVEWAY  
ACCESS SHORTENED FOR NEW PAVILION
- 14 VISTA DEL MONTE  
CO-OP APARTMENTS
- 15 DAP SUNRISE BUSINESS CENTER  
APN 507-100-026
- 16 RANCH CLUB CONDOMINIUM
- ▲ SITE ENTRY / EXIT
- EXISTING PDD NO. 281 BOUNDARY
- - - PROPOSED PDD BOUNDARY TO BE  
INCLUDED AS PART OF PDD NO. 281

PROPOSED CAMPUS CHANGES			
LAND USE REC. PARCELS (507-100-041, -042, -044, -045, -026) 13.22AC			
PDD - 281			
GENERAL PLAN	MIXED USE / MULTI USE		
ZONING	PDD 281		
DEVELOPMENT STANDARDS			
SETBACKS	F-25'	5-15' / R - 20'	(E) ANNEX F-12.5'
BUILDING HEIGHT (allowed)	30'	3-STORIES	
BUILDING HEIGHT (proposed)	30'	3 STORIES	
DENSITY (allowed)	15 DUA		
DENSITY (proposed)	12 DUA	(BASED ON 13.22 AC)	
BUILDING COVERAGE (allowed)			
BUILDING COVERAGE (proposed)	21%		
FLOOR AREA RATIO (allowed)	0.50		
FLOOR AREA RATIO (proposed)	0.32		
PROJECT DATA			
LOT AREA	13.22	SQ FT	
BUILDINGS (coverage / footprint)			
VISTA SUNRISE I APARTMENTS		19,584	
DESERT AIDS PROJECT		39,923	
DESERT AIDS PROJECT PAVILION		18,500	
SHARED PARKING, HACR		22,000	
VISTA SUNRISE II APARTMENTS (NEW SNH)		14,066	
DAP BUSINESS CENTER		8,000	
	Total building coverage	122,067	21%
BUILDINGS (total floor area)			
VISTA SUNRISE I APARTMENTS		42,872	
DESERT AIDS PROJECT & PAVILION		46,077	
DESERT AIDS PROJECT ANNEX		22,000	
VISTA SUNRISE II APARTMENTS (NEW SNH)		41,509	
DAP BUSINESS CENTER		11,145	
	Total building area F.A.R.	163,623	0.28
PARKING			
		ML	Design
MIXED USE (300 / SF +/- 20,000 SF)		268	261
VISTA SUNRISE I APARTMENTS (0.5 STALL / BED; 80 UNITS)		40	107
VISTA SUNRISE II APARTMENTS (NEW SNH) (0.5 STALL / BED; 61 UNITS)		31	31
DAP BUSINESS CENTER (BASED ON TOTAL BUILDING AREA)		37	58
	Total parking	376	457
BUILDINGS			
	NO	HGT / STY	TOT SF
	10	24' / 2.8 30' / 3	163,623
UNITS			
	STUDIO	1 BDRM	2 BDRM
VISTA SUNRISE I APARTMENTS	48	32	0
	48	12	1
	Total units		141

**CAMPUS AERIAL & PROJECT INFORMATION**  
DESERT AIDS PROJECT CAMPUS EXPANSION

1695 N. SUNRISE WAY, PALM SPRINGS, CA 92262

INTERACTIVE DESIGN CORPORATION  
199 SOUTH CIMIC DRIVE, SUITE 10, PALM SPRINGS, CA 92262  
T: 760.323.6990 E: MAIL@INTERACTIVEDESIGNCORP.COM

**SITE PLAN**

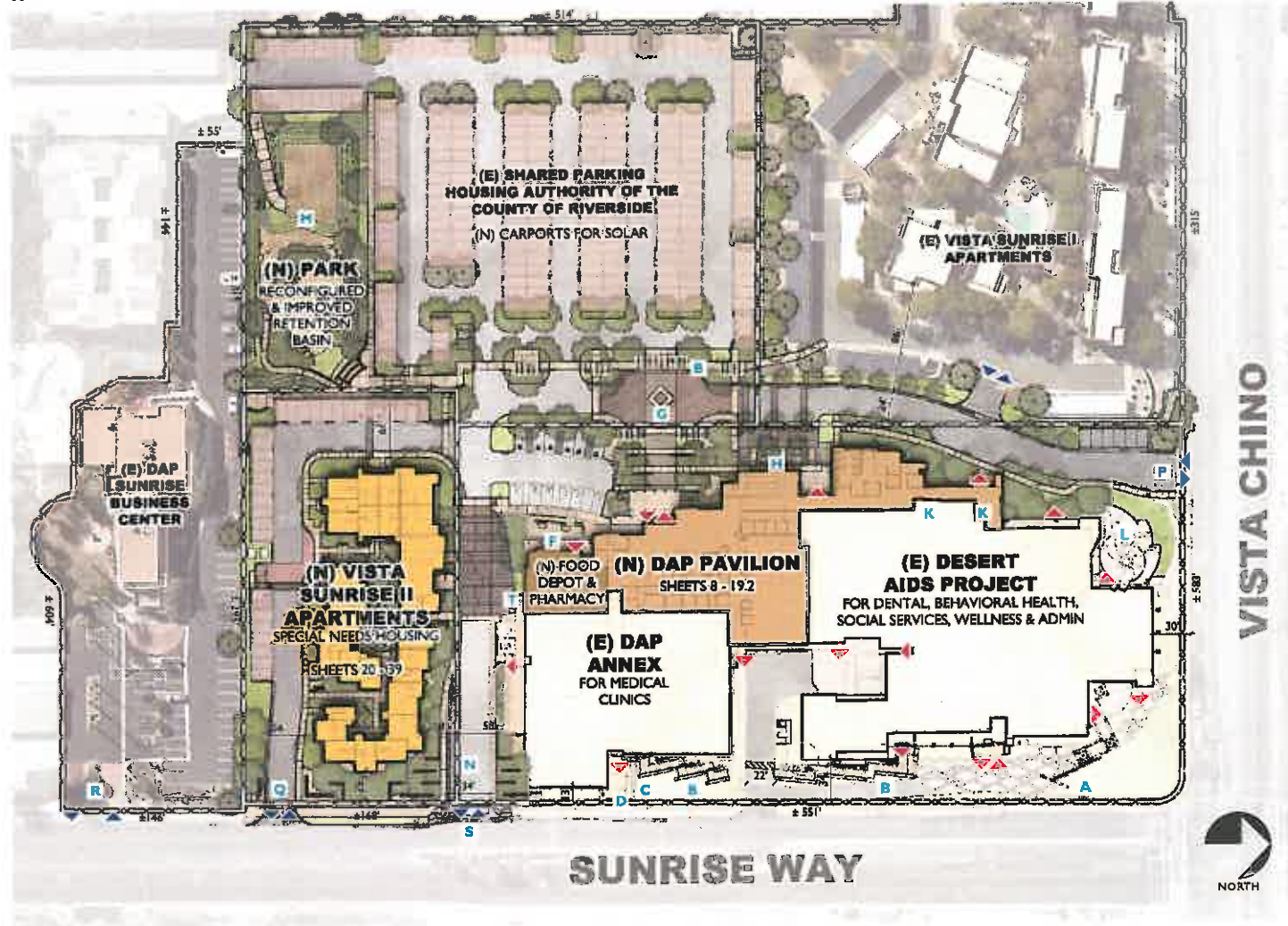
SCALE: 1" = 40'-0"

**LEGEND**

- (E) EXISTING TO REMAIN
- (N) NEW
- - - - - PROPERTY LINE
- ▲ SITE ENTRY / EXIT
- ▼ BUILDING ENTRY / EXIT
- ▽ BUILDING EXIT ONLY

**SITE AREAS**

- A (N) MONUMENT SIGNAGE W/ RETAINING CONCRETE SITE WALL
- B (N) CANOPY SHADING STRUCTURES AT WALKWAYS TO BUILDINGS
- C (E) STANDING SEAM METAL HIGH ROOF TO REMAIN
- D (N) METAL LOW ROOF FOR UPGRADED ENTRANCE TO DAP ANNEX
- E (N) ADDITION W/ RAISED PLAZA FOR FOOD DISTRIBUTION
- G (N) ARRIVAL COURT / DROP-OFF
- H (N) CAFÉ PLAZA
- K (N) GALLERY / HALLWAY CONNECTION TO (E) CORRIDOR / HALLWAY
- L (E) SERENITY GARDEN
- M (N) RECONFIGURED & IMPROVED RETENTION BASIN AS PUBLIC PARK
- N IMPROVED PROJECT ENTRY FROM SUNRISE WAY
- P IMPROVED PROJECT ENTRY FROM VISTA CHINO
- Q NEW DRIVEWAY EXIT
- R (E) DRIVEWAY ENTRY / EXIT @ DAP SUNRISE BUSINESS CENTER
- S (N) TRAFFIC LIGHT
- T (N) LOADING DOCK

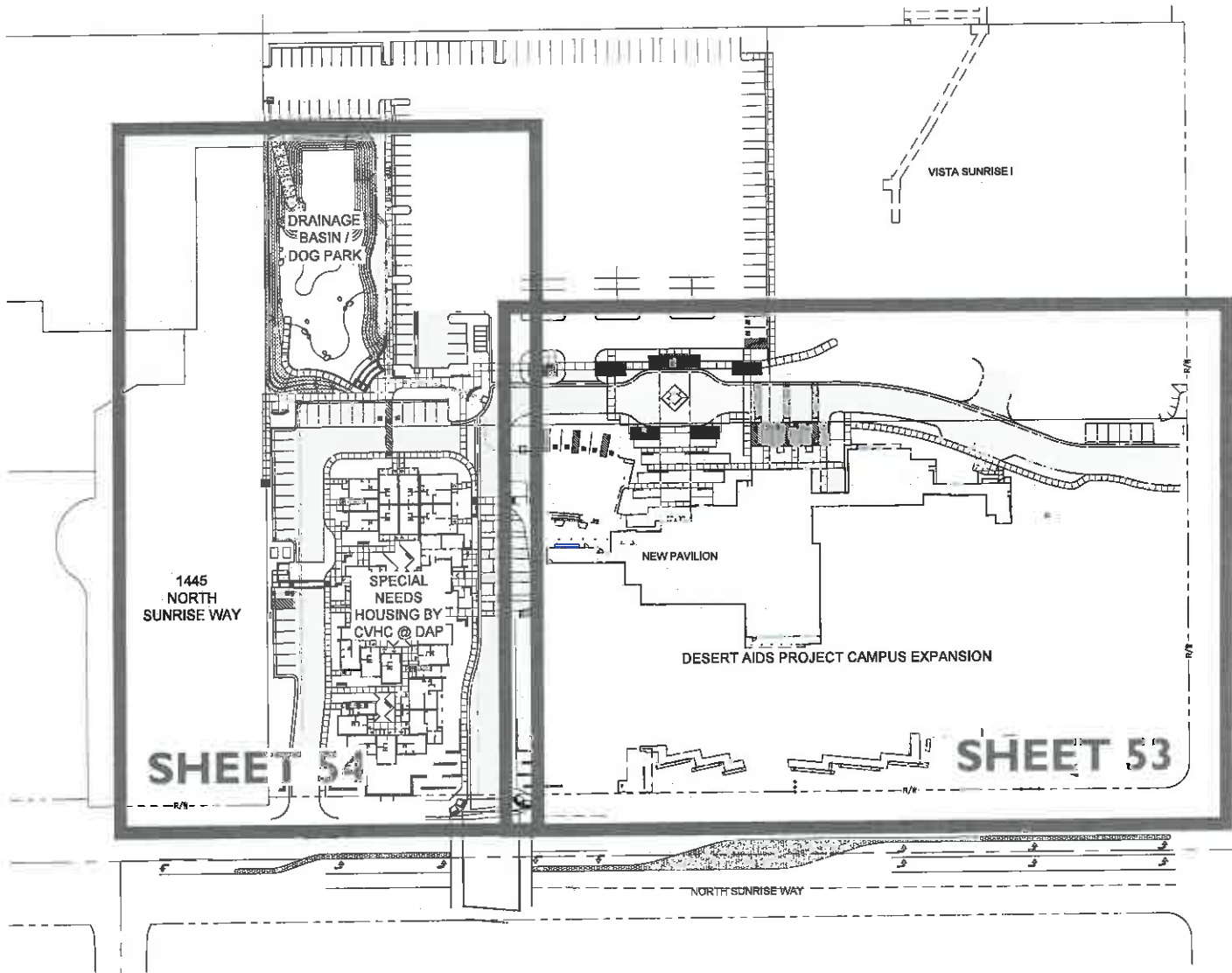


**DAP CAMPUS: SITE PLAN**  
DESERT AIDS PROJECT CAMPUS EXPANSION

1695 N. SUNRISE WAY, PALM SPRINGS, CA 92262

**INTERACTIVE DESIGN CORPORATION**  
199 SOUTH CIVIC DRIVE, SUITE 10, PALM SPRINGS, CA 92262  
760.334.4990 | EMAIL@INTERACTIVEDESIGNCORP.COM



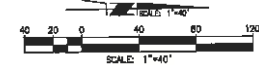


**PROJECT**  
SPECIAL NEEDS HOUSING BY CVHC @ DAP

**OWNER/DEVELOPER**  
COACHELLA VALLEY HOUSING COALITION  
45001 MARVINE STREET  
INDIO, CA 92201  
CONTACT: MARYANN YBARRA / KENNY RODGERS  
(760) 347-3157

**PROJECT**  
DESERT AIDS PROJECT CAMPUS EXPANSION

**OWNER/DEVELOPER**  
DESERT AIDS PROJECT  
1695 N. SUNRISE WAY  
PALM SPRINGS, CA 92262  
CONTACT: DAVID BRINKMAN, CEO  
(760) 323-2118



# CONCEPTUAL GRADING PLANS - INDEX SHEET

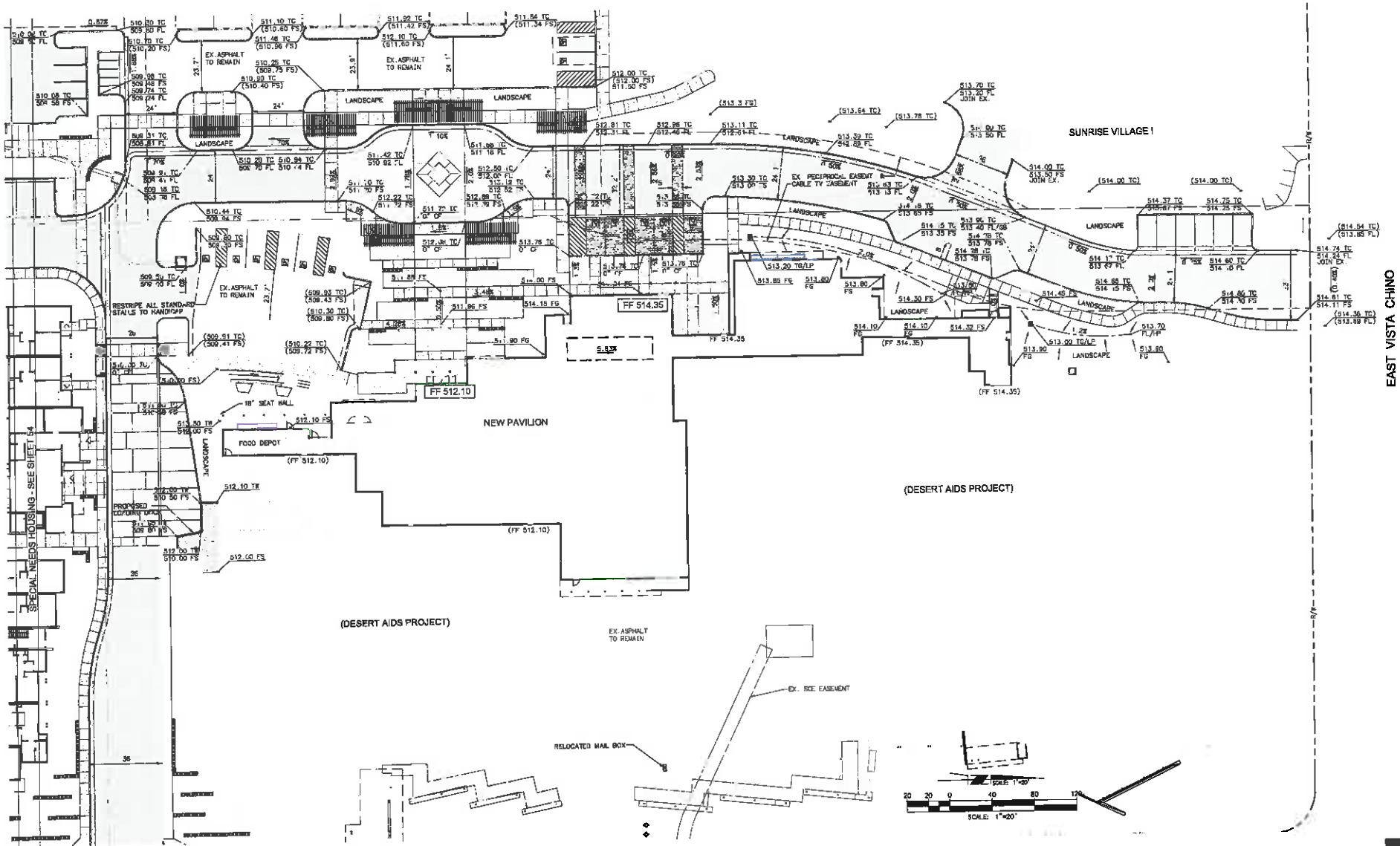
DESERT AIDS PROJECT CAMPUS EXPANSION /  
SPECIAL NEEDS HOUSING BY CVHC @ DAP

1695 N. SUNRISE WAY, PALM SPRINGS, CA 92262

Michael Baker  
INTERNATIONAL

75-410 Gerold Ford Drive, Suite 100  
Palm Desert, CA 92211  
Phone (760) 346-7481 - MBAKERINTL.COM

JN 175438  
APRIL 2020



EAST VISTA CHINO

# CONCEPTUAL GRADING PLAN

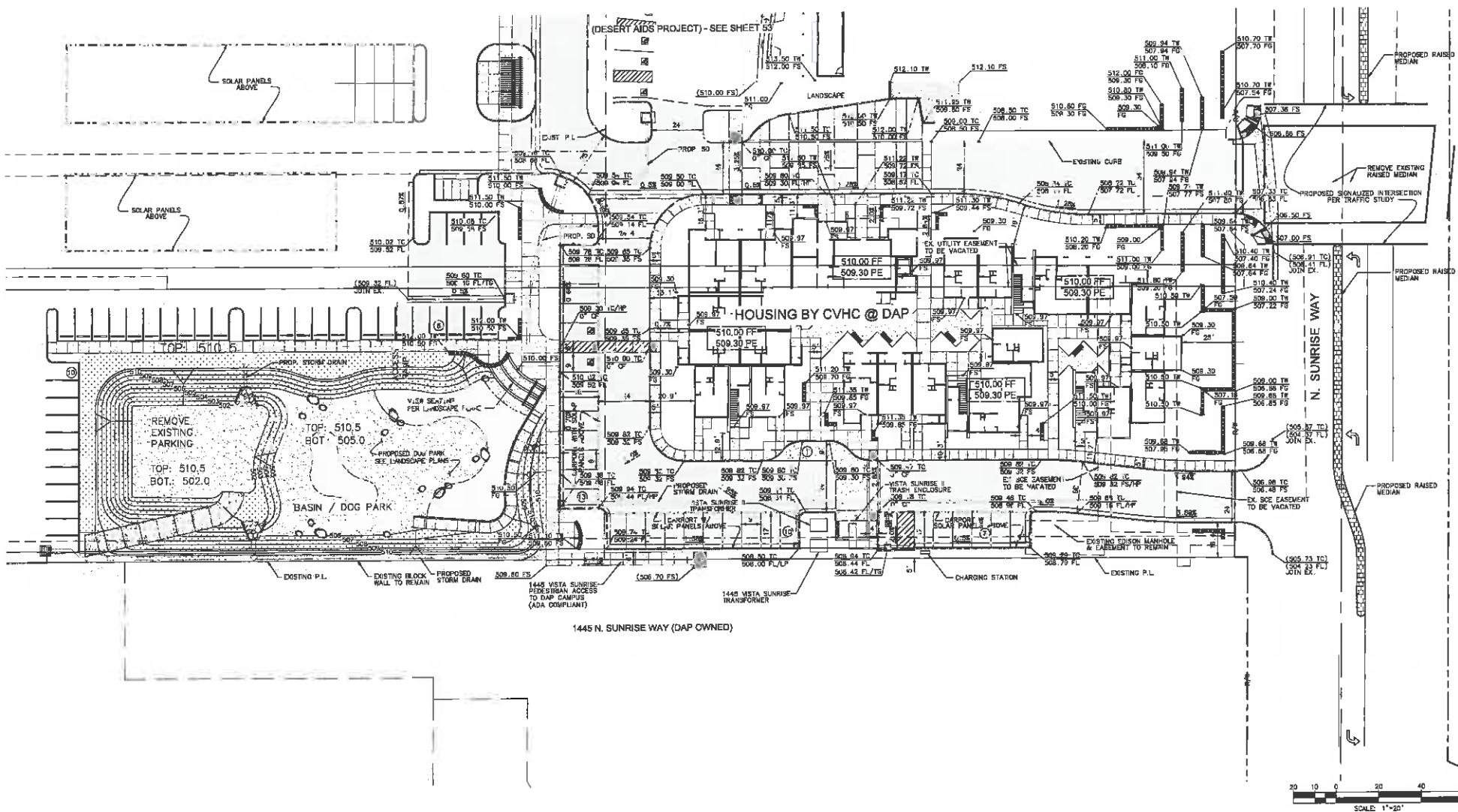
## DESERT AIDS PROJECT CAMPUS EXPANSION

1695 N. SUNRISE WAY, PALM SPRINGS, CA 92262



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JN 175438  
 APRIL 2020



# CONCEPTUAL GRADING PLAN

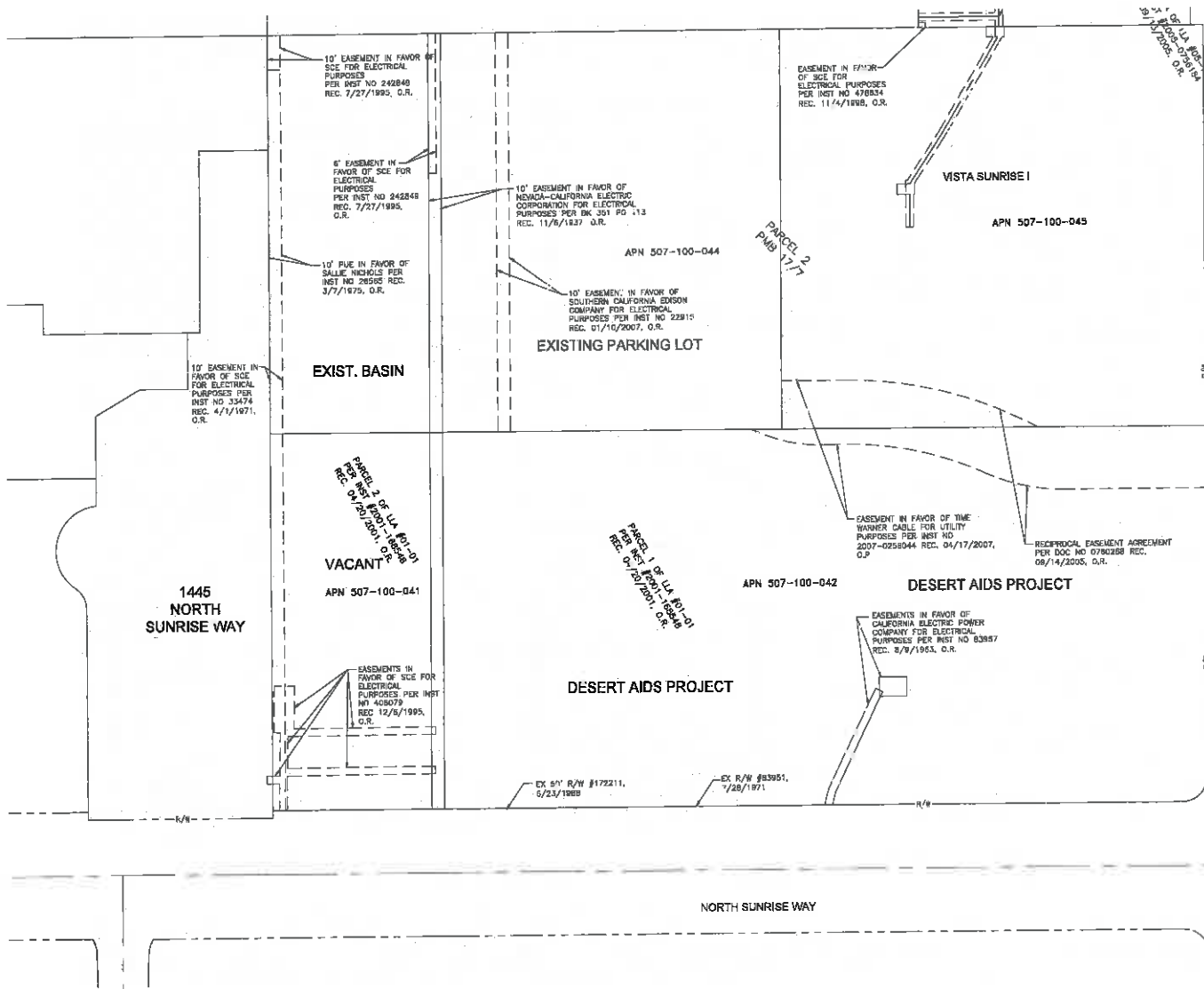
## SPECIAL NEEDS HOUSING BY CVHC @ DAP

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JOB NO. 175214  
 APRIL 2018

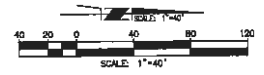


**PROJECT**  
SPECIAL NEEDS HOUSING BY CVHC @ DAP

**OWNER/DEVELOPER**  
COMPELLA VALLEY HOUSING COALITION  
45701 MONROE STREET  
INDIO, CA 92201  
CONTACT:  
WYMIAN YEMRA / JENNY RODGERS  
(760) 347-3157

**PROJECT**  
DESERT AIDS PROJECT CAMPUS EXPANSION

**OWNER/DEVELOPER**  
DESERT AIDS PROJECT  
1885 N. SUNRISE WAY  
PALM SPRINGS, CA 92262  
CONTACT:  
DAVID BRIDGMAN, CEO  
(760) 323-2118



# EXISTING CONDITIONS PLAN

DESERT AIDS PROJECT CAMPUS EXPANSION /  
SPECIAL NEEDS HOUSING BY CVHC @ DAP

1695 N. SUNRISE WAY, PALM SPRINGS, CA 92262

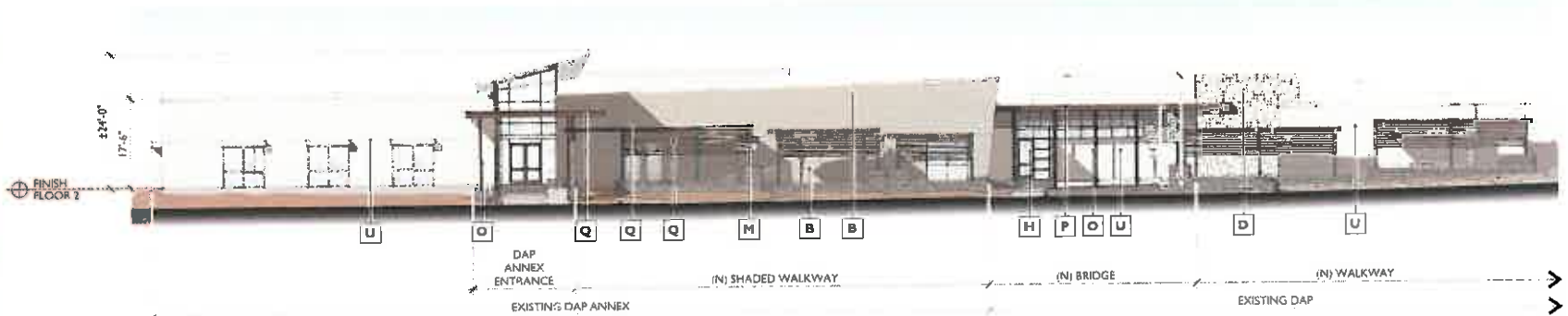


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JN 175438  
APRIL 2020



**MATERIAL LEGEND**  
REFER TO MATERIAL BOARD ON  
SHEET 16 FOR MORE INFORMATION.



SCALE: 1" = 20'-0" **PARTIAL EAST ELEVATION ②**



SCALE: 1" = 20'-0" **PARTIAL EAST ELEVATION ①**



**EAST ELEVATION**



**KEYMAP**

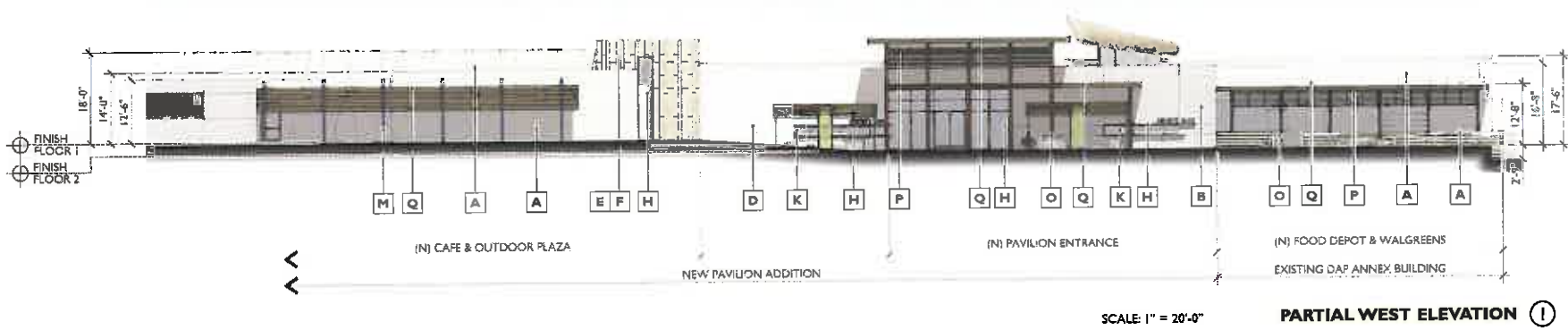
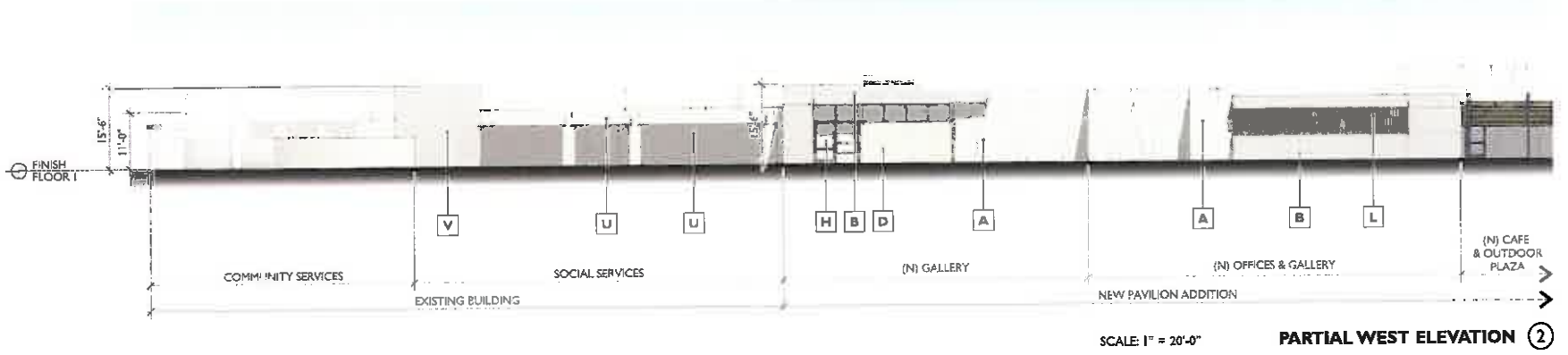
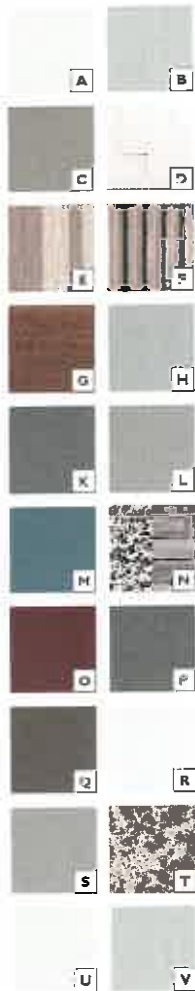
# DAP EXTERIOR ELEVATION: EAST

## DESERT AIDS PROJECT CAMPUS EXPANSION

1693 N. SUNRISE WAY, PALM SPRINGS, CA 92262

INTERACTIVE DESIGN CORPORATION  
199 SOUTH CIVIC DRIVE, SUITE 10, PALM SPRINGS, CA 92262  
760.333.4990 E: MAIL@INTERACTIVEDESIGNCORP.COM

1805.2  
MAY 15, 2020 / 234



WEST ELEVATION

**MATERIAL LEGEND**  
REFER TO MATERIAL BOARD ON SHEET 16 FOR MORE INFORMATION.



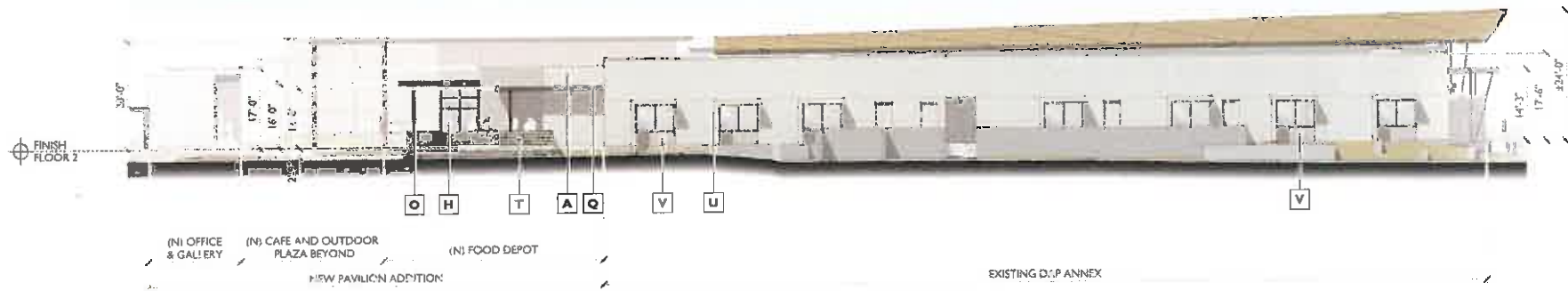
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## DESERT AIDS PROJECT CAMPUS EXPANSION

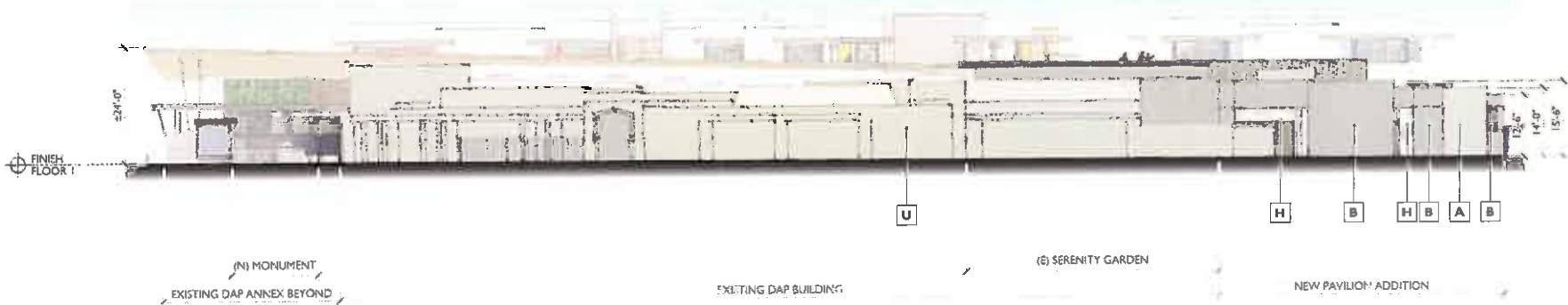
1695 N. SUNRISE WAY, PALM SPRINGS, CA 92262

INTERACTIVE DESIGN CORPORATION  
199 SOUTH CIVIC DRIVE, SUITE 10, PALM SPRINGS, CA 92252  
7-760-323-4190 E-MAIL@INTERACTIVEDESIGNCORP.COM

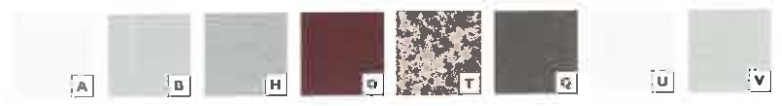
1805.2  
MAY 15, 2020 / 234



SCALE: 1" = 20'-0" **SOUTH ELEVATION ②**



SCALE: 1" = 20'-0" **NORTH ELEVATION ①**



**MATERIAL LEGEND**  
REFER TO MATERIAL BOARD ON SHEET 16 FOR MORE INFORMATION.



**KEYMAP**

## DAP CAMPUS EXPANSION BUILDING INFORMATION

### PAVILION AREAS SQUARE FOOTAGE

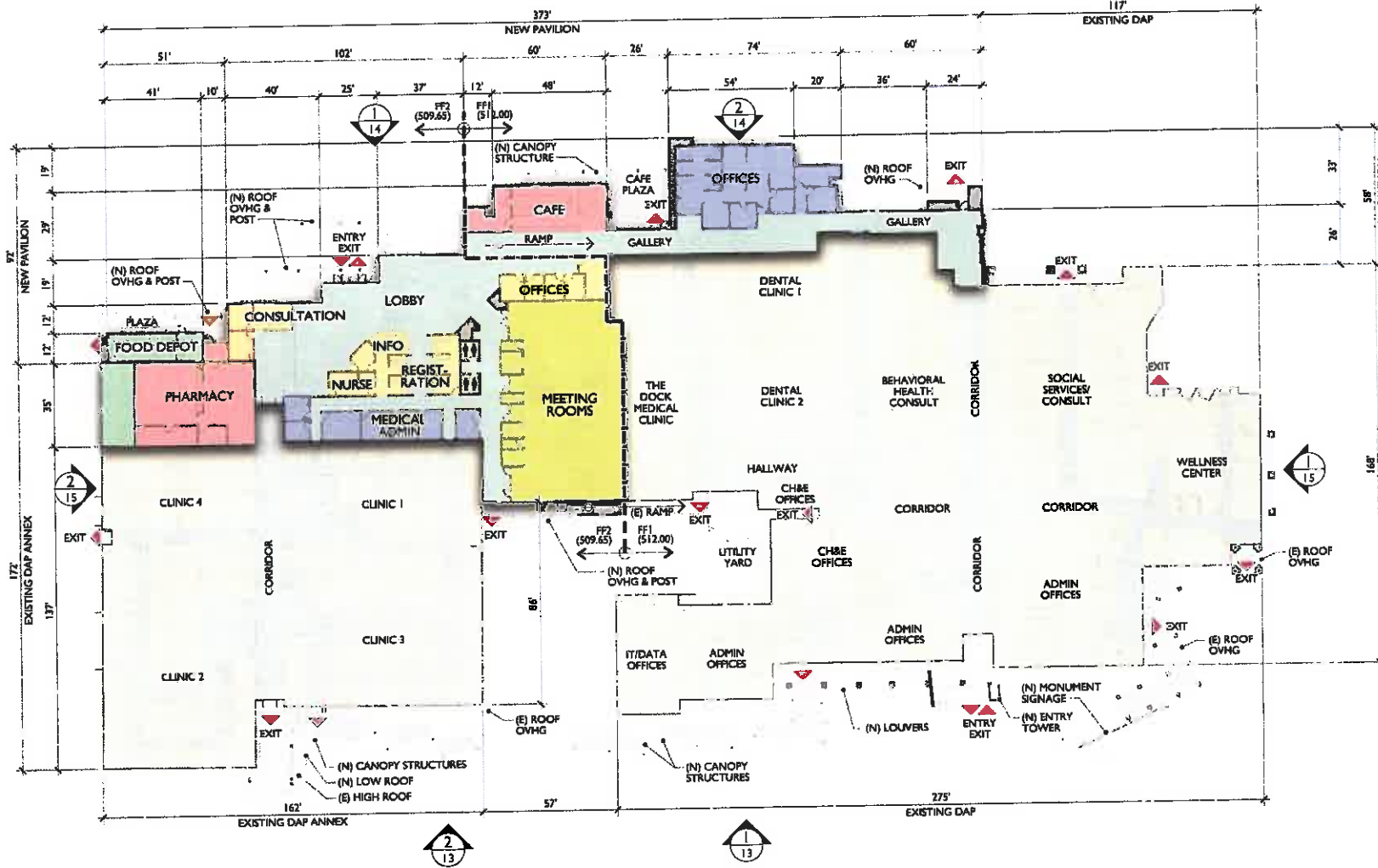
AREA	SQUARE FOOTAGE
1 <i>Consultation</i>	475
2 <i>Lobby &amp; Circulation</i>	3,560
3 <i>Nurse</i>	210
4 <i>Info</i>	290
5 <i>Registration</i>	535
6 <i>Medical Admin</i>	1,115
7 <i>Meeting Rooms</i>	4,145
8 <i>Consulting Offices</i>	610
9 <i>Café</i>	1,100
10 <i>Offices</i>	2,010
11 <i>Gallery, Circulation, &amp; Restrooms</i>	4,450
<b>TOTAL AREA</b>	<b>18,500</b>



**FLOOR PLAN** SCALE: 1" = 20'-0"

**LEGEND**

- OFFICE
- CONSULTATION
- RESTROOM
- RETAIL
- MEETING ROOM
- FOOD DEPOT
- CIRCULATION
- OVERHANGS / CANOPIES
- NOT IN CONTRACT (NIC)
- CIRCULATION (NIC)
- BUILDING ENTRY / EXIT
- BUILDING EXIT ONLY



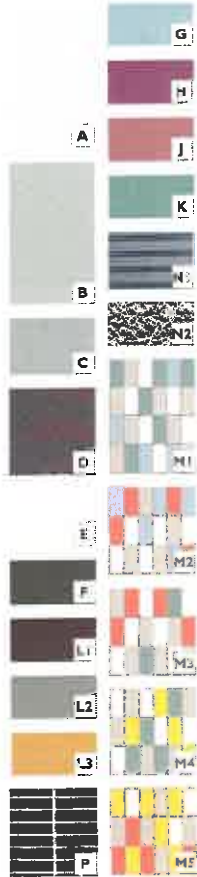
# DAP BUILDING: FLOOR PLAN

## DESERT AIDS PROJECT CAMPUS EXPANSION

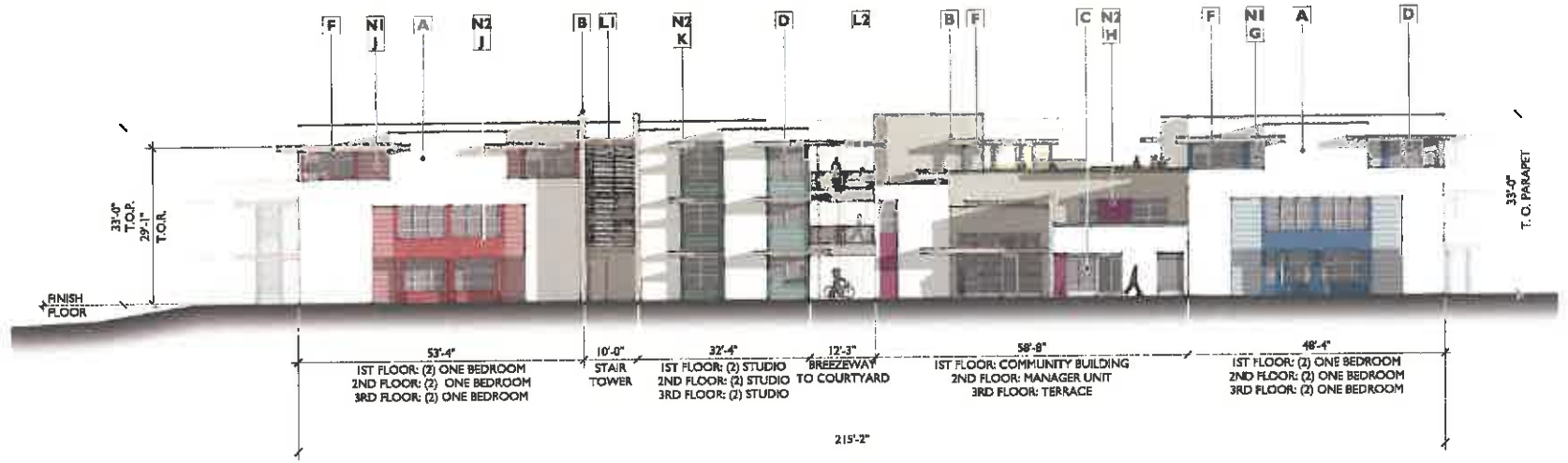
1695 N. SUNRISE WAY, PALM SPRINGS, CA 92262

INTERACTIVE DESIGN CORPORATION  
 199 SOUTH CIVIL DRIVE, SUITE 10, PALM SPRINGS, CA 92262  
 T: 760.323.4990 E: MAIL@INTERACTIVEDESIGNCORP.COM

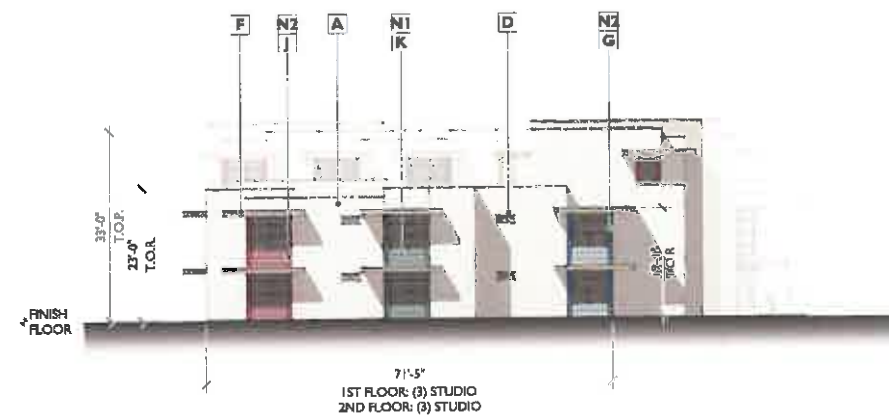
1805.2  
 MAY 15, 2020 / 234



**MATERIAL LEGEND**  
 REFER TO MATERIAL BOARD  
 ON SHEET 32 FOR MORE  
 INFORMATION.



**NORTH ELEVATION ②**



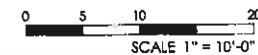
**EAST ELEVATION ①**



**FIRST FLOOR**



**NORTH & EAST EXTERIOR ELEVATIONS / MATERIAL CALLOUT**



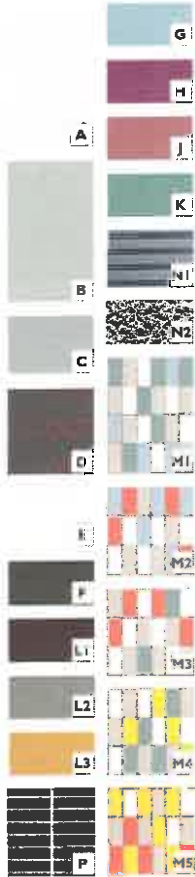
**30** of 55

**COACHELLA VALLEY HOUSING COALITION**  
**VISTA SUNRISE II APARTMENTS at DAP CAMPUS**

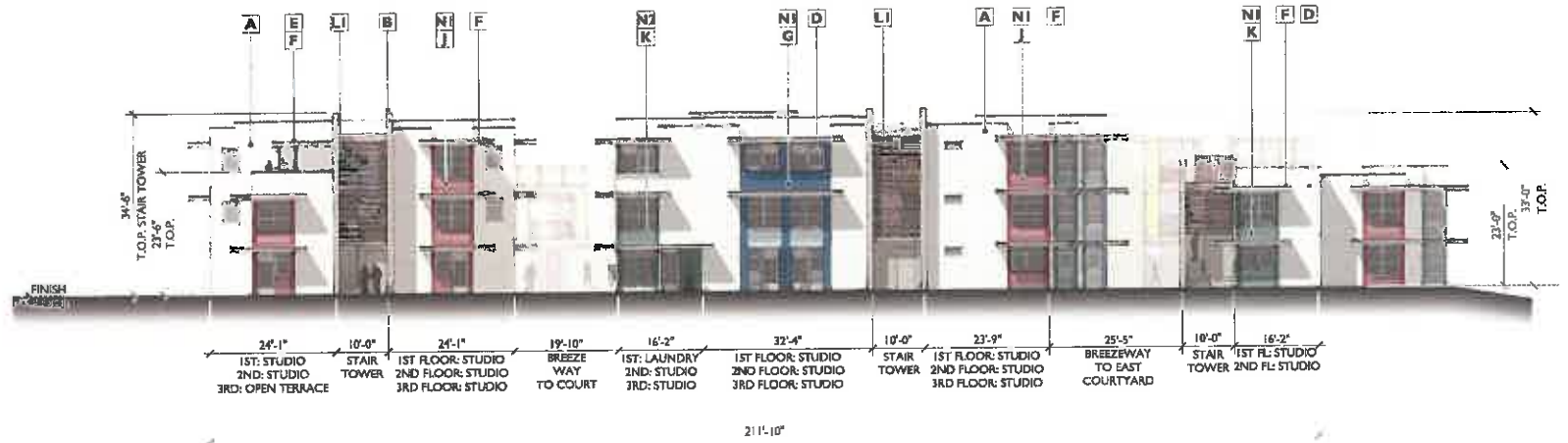
1695 N. SUNRISE WAY, PALM SPRINGS, CA 92262

**INTERACTIVE DESIGN CORPORATION**  
 199 SOUTH CIVIC DRIVE, SUITE 10, PALM SPRINGS, CA 92262  
 T: 760.325.4990 E: MAIL@INTERACTIVEDSIGN.COM

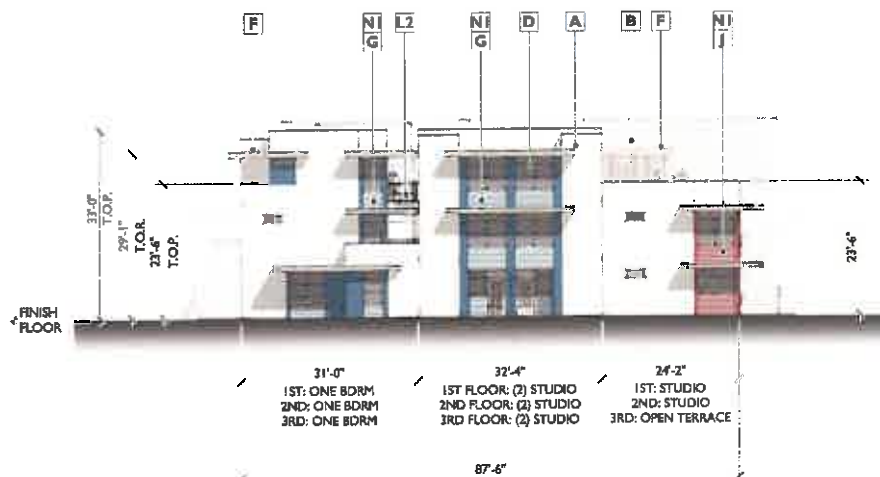
1903  
 MAY 15, 2020 / 31



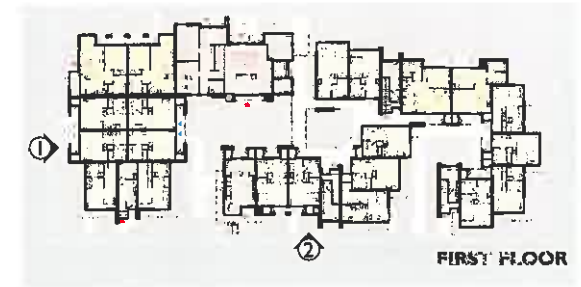
**MATERIAL LEGEND**  
 REFER TO MATERIAL BOARD  
 ON SHEET 32 FOR MORE  
 INFORMATION.



**SOUTH ELEVATION ②**



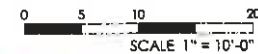
**WEST ELEVATION ①**



**FIRST FLOOR**



**SOUTH & WEST EXTERIOR ELEVATIONS / MATERIAL CALLOUT**



**31** of **55**

**COACHELLA VALLEY HOUSING COALITION**  
**VISTA SUNRISE II APARTMENTS at DAP CAMPUS**

1495 N. SUNRISE WAY, PALM SPRINGS, CA 92262

**INTERACTIVE DESIGN CORPORATION**  
 149 SOUTH CIVIC DRIVE, SUITE 10, PALM SPRINGS, CA 92262  
 T: 760.323.4990 E-MAIL: @INTERACTIVEDESIGNCOR.COM

1903  
 MAY 15, 2020 / 31

## SPECIAL NEEDS HOUSING @ DAP BY CVHC - BUILDING & UNIT DATA

### APARTMENT UNITS

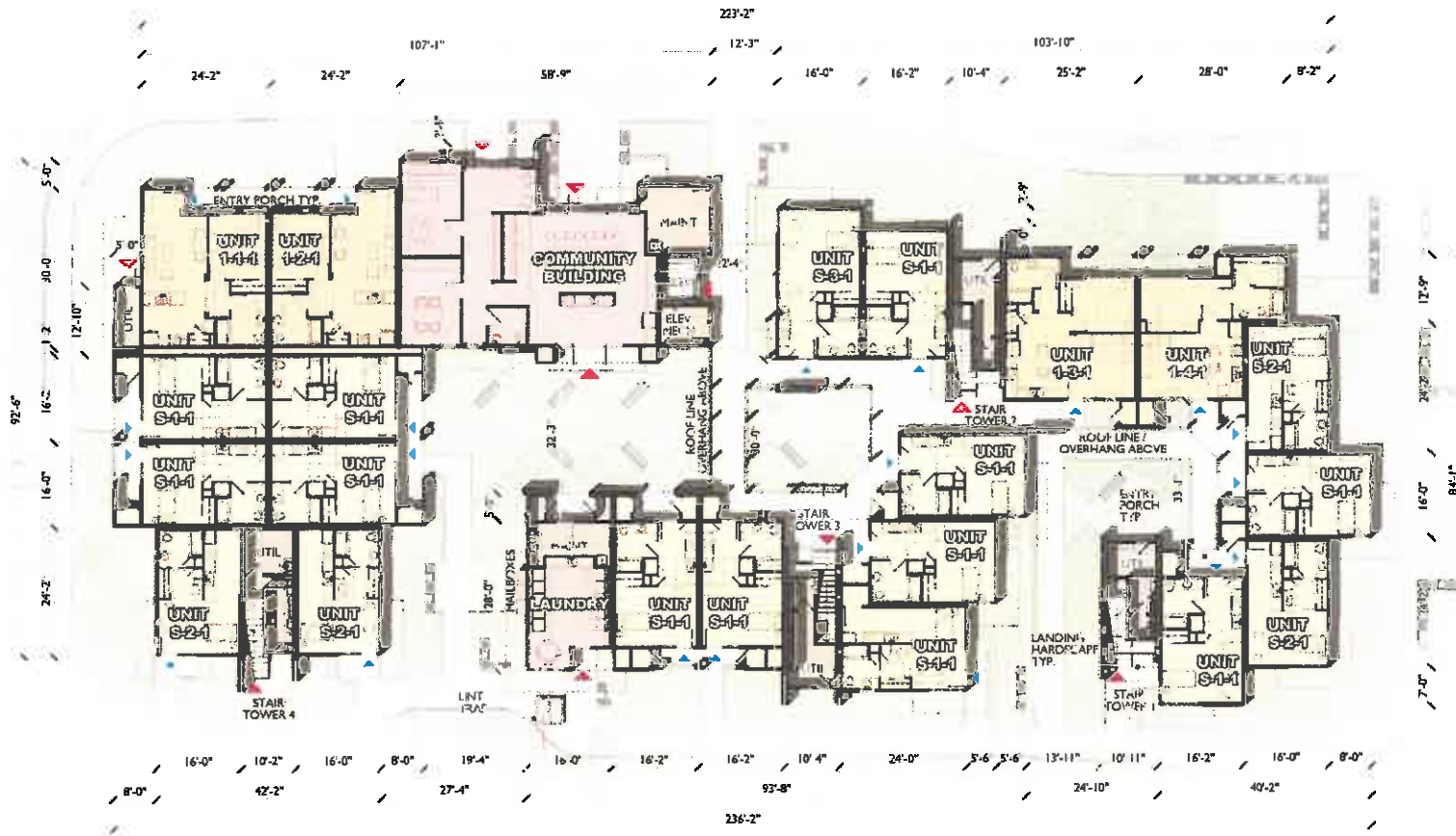
	STUDIOS (sf)				ONE BED (sf)				TWO BED (sf)		
	400	425	460	480	635	705	730	750	960	UNIT SUBTOTAL	BLDG AREA (sf)
<i>1st Floor</i>	6	5	2	4		1	2	1		21	10,280
<i>2nd Floor</i>	15		3		2	2			1	23	11,020
<i>3rd Floor</i>	10		3		2	1		1		17	8,105
<b>Total units</b>			48			12			1	61	29,405

### COMMON AREAS & SUPPORT ROOMS

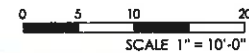
	COMMUNITY BUILDING (sf)	LAUNDRY (sf)	ELEVATOR & EQUIPMENT ROOM (sf)	TERRACES (sf)	CIRCULATION (CORRIDOR BALCONIES / STAIRS) & UTIL ROOMS (sf)	BLDG AREA (sf)
<i>1st Floor</i>	1,600	530	150		1,500	3,780
<i>office mgr</i>	(190)					
<i>lobby</i>	(110)					
<i>multi-purpose rm</i>	(555)					
<i>unisex restroom</i>	(45)					
<i>case mgr</i>	(175)					
<i>2nd Floor</i>			200		4,098	4,298
<i>3rd Floor</i>			450	1,100	3,576	5,126
	1,600	530	800	1,100	9,174	13,204
<b>TOTAL AREA</b>						<b>42,609</b>

**LEGEND**

- ▲ COMMON ACCESS
- ▲ UNIT ACCESS
- STUDIO UNIT
- ONE BEDROOM UNIT
- TWO BEDROOM MANAGER UNIT
- COMMUNITY AREA
- ENTRY PORCH / PATIO / CIRCULATION
- UTILITY / MAINTENANCE
- ROOF
- ROOF LINE / CORRIDOR BALCONY ABOVE
- # OF ROOMS IN UNIT
- S-1-1 STORY ID
- UNIT TYPE



NORTH



**22** of 55

**FIRST FLOOR PLAN**

**COACHELLA VALLEY HOUSING COALITION**  
**VISTA SUNRISE II APARTMENTS at DAP CAMPUS**

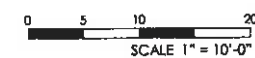
1695 N. SUNRISE WAY, PALM SPRINGS, CA 92262

INTERACTIVE DESIGN CORPORATION  
 149 SOUTH CIVIC DRIVE, SUITE 10, PALM SPRINGS, CA 92262  
 T 760.323.4990 E MAIL@INTERACTIVEDESIGNCORP.COM

1903  
 MAY 13, 2023/31

**LEGEND**

- ▲ COMMON ACCESS
- ▲ UNIT ACCESS
- STUDIO UNIT
- ONE BEDROOM UNIT
- TWO BEDROOM MANAGER UNIT
- COMMUNITY AREA
- ENTRY PORCH / PATIO / CIRCULATION
- UTILITY / MAINTENANCE
- ROOF
- ROOF LINE / CORRIDOR BALCONY ABOVE
- # OF ROOMS IN UNIT
- S-111 STORY ID
- UNIT TYPE



**23** of 55

**SECOND FLOOR PLAN**

**COACHELLA VALLEY HOUSING COALITION**  
**VISTA SUNRISE II APARTMENTS at DAP CAMPUS**

1595 N. SUNRISE WAY, PALM SPRINGS, CA 92262

**INTERACTIVE DESIGN CORPORATION**  
 199 SOUTH CMC DRIVE, SUITE 10, PALM SPRINGS, CA 92262  
 P: 760.321.4990 E: MAIL@INTERACTIVEDESIGNCORP.COM

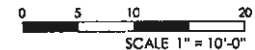
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 MAY 15, 2020 / 31



THIRD FLOOR PLAN

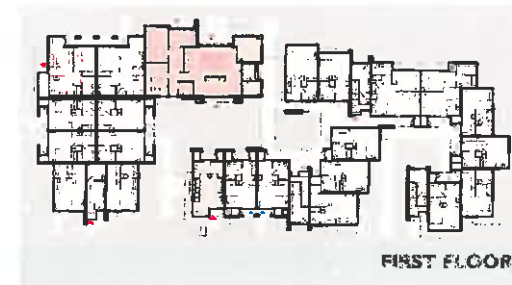
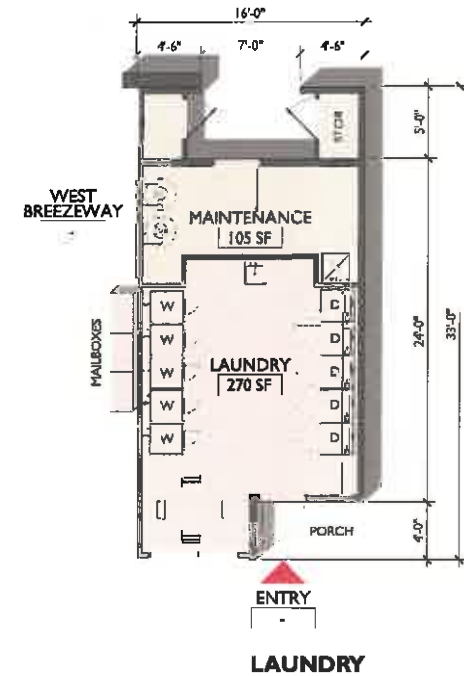
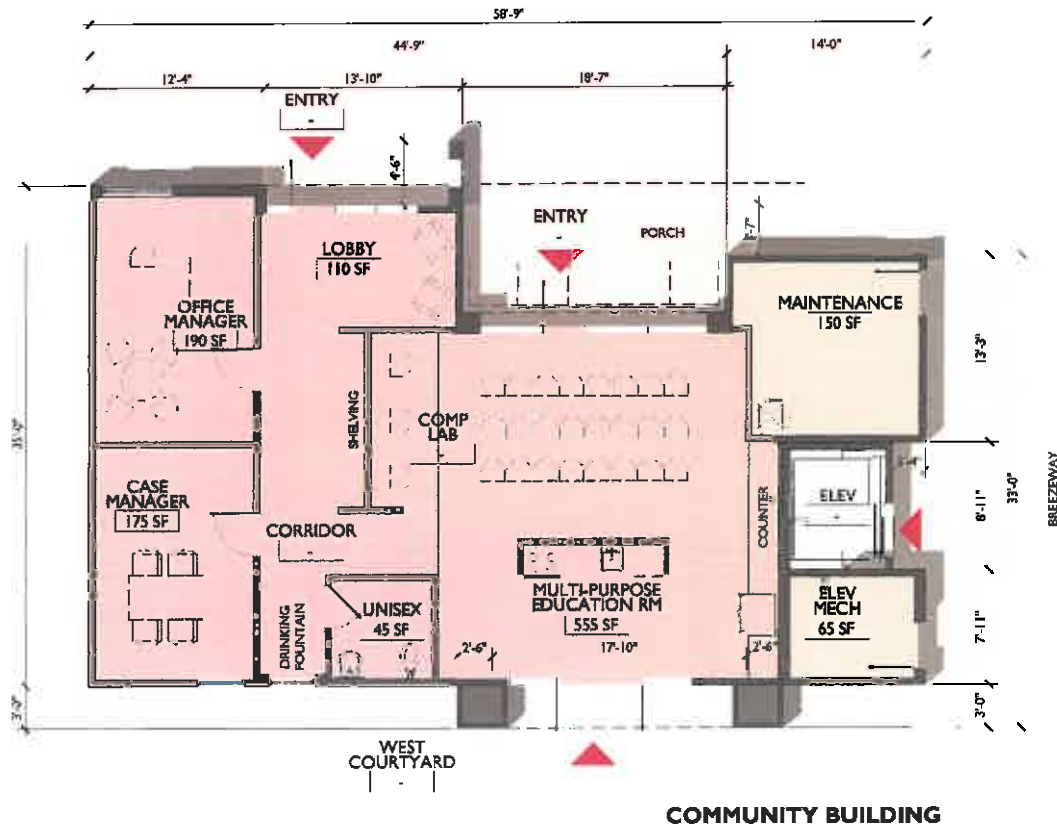
**COACHELLA VALLEY HOUSING COALITION**  
**VISTA SUNRISE II APARTMENTS at DAP CAMPUS**

1695 N. SUNRISE WAY, PALM SPRINGS, CA 92262



INTERACTIVE DESIGN CORPORATION  
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 MAY 15, 2020 / 31



**FLOOR PLANS - COMMUNITY BUILDING & LAUNDRY**

**COACHELLA VALLEY HOUSING COALITION**  
**VISTA SUNRISE II APARTMENTS at DAP CAMPUS**

1695 N. SUNRISE WAY, PALM SPRINGS, CA 92262

**INTERACTIVE DESIGN CORPORATION**  
 199 SOUTH CIVIC DRIVE, SUITE 10, PALM SPRINGS, CA 92262  
 T 760.323.4990 E MAIL@INTERACTIVEDSIGNCORP.COM





June 18, 2020

Mr. Paul Rull  
Riverside County Airport Land Use Commission  
4080 Lemon Street, 14<sup>th</sup> Floor  
Riverside, CA 92501

RE: Desert AIDS Project Campus Expansion Project, Wildlife Hazards

Dear Paul:

This letter has been prepared in response to your questions regarding the retention basin planned for the above-referenced project, and its potential to attract large flocks of birds which would pose a hazard to airport operations at Palm Springs International Airport.

### **Project Description and Existing Conditions**

The proposed Project includes the addition of an 18,500 square foot medical office building which will connect the two existing Desert AIDS Project (DAP) buildings, and a 61 unit special needs housing project. A 12,000± square foot retention basin which has existed since at least 2009 (Google Earth image shown here, 6/2009) when the second DAP building was constructed currently occurs on the site. As part of the proposed Project, it will be enlarged to 17,700 square feet, and re-landscaped as a dog park for the residents of the special needs housing, and the existing Vista Sunrise Apartment residents west of the DAP buildings (please refer to the project plan set we provided under separate cover). There has been no incident of flocks of birds being attracted to the existing basin since its construction 11 years ago, nor has the basin's existence contributed to bird strikes at the Palm Springs International Airport.



## **Relationship to Airport Land Use Commission Wildlife Hazard Management Planning**

First and foremost, it is important to note that the expanded basin is designed for retention, not detention. The former, by definition, gathers storm flows from a site, concentrates them in a basin, and allows them to immediately drain through the proposed decomposed granite, into the subsoil. A detention basin, on the other hand, is designed to hold stormwater for undetermined periods of time, until the storm drain to which it is connected has capacity to accommodate the flow.

The existing basin is currently landscaped, and L-shaped. The reconfigured basin will be rectangular, and will contain landscaping on its side slopes, and decomposed granite on the bottom. The primary use will be as a dog park, as the Coachella Valley experiences less than 5 inches of rain a year, on average. Further, the retention basin is required by the City's standards and requirements to drain within 72 hours of any storm event. It will never have long-term standing water.

Further, the basin is being designed as a dog park, not as passive open space. As a result, there will be regular human activity within it, which will discourage its use by wildlife for nesting and foraging.

Table 5-2 of *Wildlife Hazard Management at Riverside County Airports: Background and Policy* identifies "Policy Recommendation(s)" for residential subdivisions. The proposed project does not include or involve a residential subdivision, but rather an urban apartment project at a density of more than 40 units per acre. Neither the apartments nor the retention basin will include "picnic areas, extended detention ponds, (or) fountains," and are therefore consistent with that recommendation. We have attached the project's preliminary hydrology study for your review, to demonstrate that the basin will be designed to drain the 100 year storm, not to create a pond, to address the second policy recommendation. We will agree to a "No Feeding" policy to be promulgated and posted at the basin's entry, to address the third policy recommendation.

According to Table 5-4, the proposed basin is an "Infiltration Basin." It will drain in 72 hours, consistent with City and County standards, when a 100-year storm event occurs. For most storms, which are far smaller than the 100-year storm in both intensity and volume, the basin will drain in less than 72 hours. Given that the Coachella Valley experiences storm events once or twice a year, the basin will not contain water for any length of time that would result in its being an attractant to hazardous bird species, including geese. As previously described, the existing basin has existed for 11 years, and has never resulted in a population of resident or migratory birds nesting or foraging in flocks.

In summary, the existing retention basin has not been an attractant for birds in the last 11 years, and the enlarged basin, which will be identical in function, will not change that condition. The local environment does not generate sufficient rain to create standing

water conditions, nor do storm events occur regularly to create the potential for ponding in the desert environment. The proposed expansion of the existing retention basin will not result in any hazard to the Palm Springs International Airport from increased bird activity.

Please feel free to contact me if you have any questions.

Sincerely,

A handwritten signature in blue ink, appearing to read "Nicole Sauviat Criste". The signature is fluid and cursive, with the first name "Nicole" being the most prominent.

Nicole Sauviat Criste  
Principal

**PRELIMINARY DRAINAGE STUDY  
FOR  
DESERT AIDS CAMPUS EXPANSION**

Date: April 27, 2020

*Prepared for:*  
Coachella Valley Housing Coalition  
45701 Monroe Street #G  
Indio CA 92201  
Phone: 760-347-3157

And

Desert AIDS Project  
1695 N Sunrise Way  
Palm Springs, CA 92262  
Phone: 760-323-2118

<i>Revision History</i>	
Date	Comment

*Report Prepared By:*

Michael Baker International  
75-410 Gerald Ford Drive, Suite 100  
Palm Desert, CA. 92211  
Phone: 760-346-7481  
Fax: 760-346-8315

MBI JN 175261



*Engineer of Work/ Contact Person:*  
Prasad Kasturi, P.E.

## **SECTION 1 – INTRODUCTION**

### **1.1 BACKGROUND**

Michael Baker International has been retained by Coachella Valley Housing Coalition and Desert AIDS Project to prepare Preliminary Engineering Design for expansion of Desert AIDS medical facility and an apartment complex and a drainage report to show how storm water runoff from the 100-year storm event shall be handled on-site for proper storm water handling and safety to the public.

The project site is in the City of Palm Springs, California at the southwest corner of Vista Chino Road and Sunrise Way. Figure 1 shows the general vicinity of the project location.

The approximately 5.5-acre site has an existing medical building and associated parking lot and currently lies within a FEMA mapped flood plain Zone X, Areas determined to be outside the 0.2% annual chance floodplain. The site has an existing retention basin near the southwest corner to retain and infiltrate storm water. Vacant lot about 1-acre to the east of the basin is part of the site and will be converted into apartments. Due to the increase in impervious area, the retention basin will be increased in size to accept addition flows from the site. New inlets and storm drain are proposed to safely discharge the runoff to the retention basin.

### **1.2 OBJECTIVE**

1.) The objective of this drainage study is to determine the 100-year onsite discharges in accordance with the criteria and procedures described in accordance with Riverside County Flood Control & Water Conservation District (RCFCD&WCD) Hydrology Manual. The included calculations have been prepared to show the required storage volume for on-site retention.

2.) As of June 15, 2009, developers must comply with the Colorado River Basin Regional Water Quality Control Board (CRBRWCB) requiring the preparation, approval, and implementation of a project-specific WQMP for discretionary New Developments and Redevelopment projects that fall into one (1) of the eight (8) Priority Development Project categories. This project falls into category 7; "Home subdivisions with 10 or more housing units". A separate submittal will be made for the Final project-specific WQMP.

## **SECTION 2 – HYDROLOGY**

### **2.1 APPROACH AND METHODOLOGY**

The hydrologic analysis described in this report was performed in accordance with the criteria and procedures outlined in the Riverside County Flood Control and Water Conservation District Hydrology Manual dated April 1978, referred to hereafter as "Hydrology Manual".

The Hydrology Manual Soil Map, Plate C-1.35 shows the site lies entirely within hydrologic soil type "A". A copy of said Plate is included herewith.

Precipitation data used in this study was taken from NOAA Atlas 14 website at the project site and included in Appendix A.

The developed site consists more than 90% impervious cover and is considered commercial land use. See RCFC&WCD Plate D-5.6 included in Appendix A.

## **2.2 RATIONAL METHOD HYDROLOGY RESULTS**

The peak 100-year discharges were calculated using the rational method, as prescribed by the RCFC&WCD Hydrology Manual. Detailed calculations are included herewith under Rational Method Calculations in Appendix A. Catch basin inlet and storm drain pipe size calculations (Hydraulics) will be provided at final hydrology and hydraulics report. A summary can be found on proposed condition hydrology map (Exhibit A).

## **2.3 STORAGE VOLUMES**

The project is required to retain 100% of the 100-year storm event onsite. A spreadsheet based on the Shortcut Method Synthetic Unit Hydrograph approach as prescribed by the RCFC&WCD Hydrology Manual has been utilized to perform the calculations. Basin storage capacity is modeled based on manual integration of the basin area at 1-foot contour intervals. Rainfall input data for the 100-year, 3-hour, 6-hour, and 24-hour storms is input per said Hydrology Manual. Basin Inflow is modeled in 5-minute intervals for the 3-hour and 6-hour storms, and 15-minute intervals for the 24-hour storm, based on the design storm unit hydrographs presented in the manual.

The full calculations can be found in the Synthetic Unit Hydrograph Calculations Section of this Report. A summary is provided below:

Basin Name:	Basin "A"		
Top (WSE):	510.0		
Depth:	8 Feet		
Area @ top:	17,197 sq. ft.		
Required Flood Volume:	60,675 Cu Ft (1.39 Ac-Ft)		
Provided Flood Volume:	61,181 Cu Ft (1.40 Ac-Ft)		
100 Year Water Surface:	3 Hour: 507.31	6 Hour: 507.63	24 Hour: 509.07

## **2.4 DEVELOPMENT TYPE**

The proposed development is based upon commercial land use with medical facility and high-density apartment homes.

## **2.5 DRAINAGE AREA AND FLOW PATTERNS**

The proposed development provides for one retention basin on the southwest corner of the site. Therefore, the proposed site grading and drainage areas drain towards the retention basin. The collection locations and catch basin sizes carefully planned to proportionately collect and convey runoff for retention and eventual drawdown of the storm water volume generated from the site. Exhibit "A" (Hydrology map) in the appendix clearly

details the various subareas and collection systems to be employed.

### **2.5.1 OFF-SITE DRAINAGE AREA AND FLOW PATTERNS**

There are no offsite drainage areas that drain towards the site and therefore the impact to the site from offsite areas is non-existent.

### **2.6 MANNING'S EQUATION "N" VALUES**

Manning's "N" values for asphalt with a smooth texture are typically taken as 0.015. N values for troweled finished Concrete Gutter are typically taken as 0.015. It was considered appropriate to utilize a manning's value of 0.015 for all curb/gutter/pavement cross sections.

## **SECTION 3 – CONCLUSION**

The methodologies used in this study follow the City of Palm Springs and Riverside County Flood Control and Water Conservation District Criteria.

The project site lies within FEMA designated Zone X, Areas determined to be outside the 0.2% annual chance floodplain. There are no anticipated negative downstream or upstream impacts.

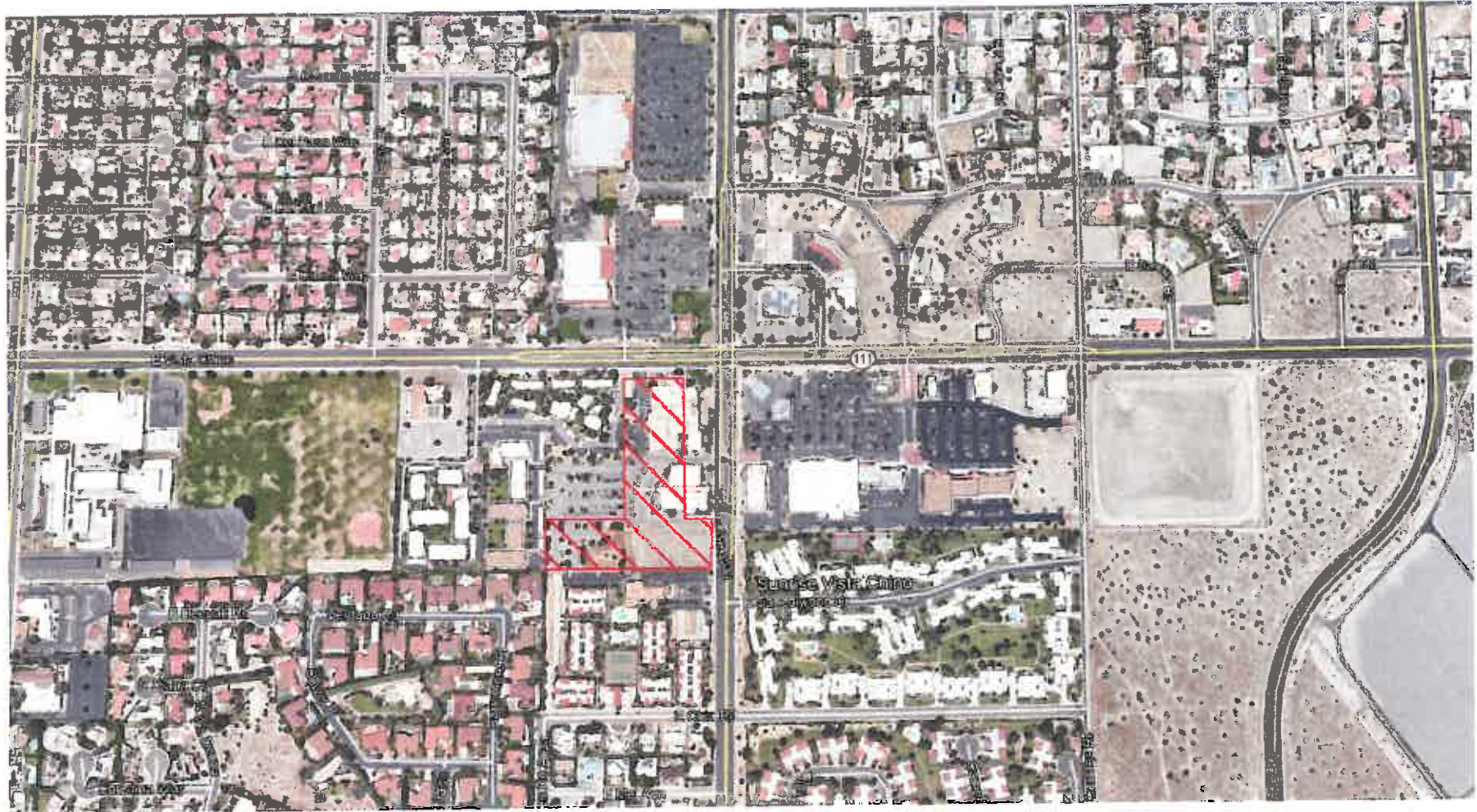
## **SECTION 4 – REFERENCES**

1. Riverside County Flood Control and Water Conservation District Hydrology Manual, 1978.
2. CivilDesign – Rational Method calculation software
3. Hydraflow Express Extension for Civil 3D, Hydraulic sizing/calculation program

# VICINITY MAP



**Figure 1. Vicinity Map**



**Legend**

 **Project Location**

# **RATIONAL METHOD CALCULATIONS**

Riverside County Rational Hydrology Program

CIVILCADD/CIVILDESIGN Engineering Software, (c) 1989 - 2014 Version 9.0  
Rational Hydrology Study Date: 04/24/20 File:DAP10YR.out

-----  
DESERT AIDS/CVHC HYDROLOGY CALCULATIONS  
10-YR RATIONAL METHOD ANALYSIS  
PROPOSED CONDITION HYDROLOGY  
BY PKASTURI APRIL 2020  
-----

\*\*\*\*\* Hydrology Study Control Information \*\*\*\*\*

English (in-lb) Units used in input data file  
-----

Program License Serial Number 6388  
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Rational Method Hydrology Program based on  
Riverside County Flood Control & Water Conservation District  
1978 hydrology manual

Storm event (year) = 10.00 Antecedent Moisture Condition = 2

Standard intensity-duration curves data (Plate D-4.1)  
For the [ Palm Springs ] area used.  
10 year storm 10 minute intensity = 2.830 (In/Hr)  
10 year storm 60 minute intensity = 1.000 (In/Hr)  
100 year storm 10 minute intensity = 4.520 (In/Hr)  
100 year storm 60 minute intensity = 1.600 (In/Hr)

Storm event year = 10.0  
Calculated rainfall intensity data:  
1 hour intensity = 1.000 (In/Hr)  
Slope of intensity duration curve = 0.5800

+++++  
Process from Point/Station 10.000 to Point/Station 20.000  
\*\*\*\* INITIAL AREA EVALUATION \*\*\*\*

-----  
Initial area flow distance = 575.000 (Ft.)  
Top (of initial area) elevation = 514.240 (Ft.)  
Bottom (of initial area) elevation = 508.660 (Ft.)  
Difference in elevation = 5.580 (Ft.)  
Slope = 0.00970 s (percent) = 0.97  
TC =  $k(0.300) * [(length^3) / (elevation\ change)]^{0.2}$   
Initial area time of concentration = 9.629 min.  
Rainfall intensity = 2.890 (In/Hr) for a 10.0 year storm  
COMMERCIAL subarea type  
Runoff Coefficient = 0.855  
Decimal fraction soil group A = 1.000  
Decimal fraction soil group B = 0.000  
Decimal fraction soil group C = 0.000  
Decimal fraction soil group D = 0.000  
RI index for soil (AMC 2) = 32.00  
Pervious area fraction = 0.100; Impervious fraction = 0.900  
Initial subarea runoff = 7.218 (CFS)  
Total initial stream area = 2.920 (Ac.)  
Pervious area fraction = 0.100

+++++  
Process from Point/Station 20.000 to Point/Station 20.000  
\*\*\*\* SUBAREA FLOW ADDITION \*\*\*\*

-----  
COMMERCIAL subarea type  
Runoff Coefficient = 0.855  
Decimal fraction soil group A = 1.000  
Decimal fraction soil group B = 0.000  
Decimal fraction soil group C = 0.000  
Decimal fraction soil group D = 0.000  
RI index for soil (AMC 2) = 32.00

Pervious area fraction = 0.100; Impervious fraction = 0.900  
Time of concentration = 9.63 min.  
Rainfall intensity = 2.890(In/Hr) for a 10.0 year storm  
Subarea runoff = 0.964(CFS) for 0.390(Ac.)  
Total runoff = 8.182(CFS) Total area = 3.310(Ac.)

\*\*\*\*\*  
Process from Point/Station 20.000 to Point/Station 30.000  
\*\*\*\* PIPEFLOW TRAVEL TIME (Program estimated size) \*\*\*\*

---

Upstream point/station elevation = 506.000(Ft.)  
Downstream point/station elevation = 505.000(Ft.)  
Pipe length = 50.00(Ft.) Manning's N = 0.013  
No. of pipes = 1 Required pipe flow = 8.182(CFS)  
Nearest computed pipe diameter = 15.00(In.)  
Calculated individual pipe flow = 8.182(CFS)  
Normal flow depth in pipe = 11.09(In.)  
Flow top width inside pipe = 13.17(In.)  
Critical Depth = 13.49(In.)  
Pipe flow velocity = 8.42(Ft/s)  
Travel time through pipe = 0.10 min.  
Time of concentration (TC) = 9.73 min.

\*\*\*\*\*  
Process from Point/Station 30.000 to Point/Station 30.000  
\*\*\*\* SUBAREA FLOW ADDITION \*\*\*\*

---

COMMERCIAL subarea type  
Runoff Coefficient = 0.855  
Decimal fraction soil group A = 1.000  
Decimal fraction soil group B = 0.000  
Decimal fraction soil group C = 0.000  
Decimal fraction soil group D = 0.000  
RI index for soil(AMC 2) = 32.00  
Pervious area fraction = 0.100; Impervious fraction = 0.900  
Time of concentration = 9.73 min.  
Rainfall intensity = 2.873(In/Hr) for a 10.0 year storm  
Subarea runoff = 1.081(CFS) for 0.440(Ac.)  
Total runoff = 9.263(CFS) Total area = 3.750(Ac.)

\*\*\*\*\*  
Process from Point/Station 30.000 to Point/Station 30.000  
\*\*\*\* SUBAREA FLOW ADDITION \*\*\*\*

---

COMMERCIAL subarea type  
Runoff Coefficient = 0.855  
Decimal fraction soil group A = 1.000  
Decimal fraction soil group B = 0.000  
Decimal fraction soil group C = 0.000  
Decimal fraction soil group D = 0.000  
RI index for soil(AMC 2) = 32.00  
Pervious area fraction = 0.100; Impervious fraction = 0.900  
Time of concentration = 9.73 min.  
Rainfall intensity = 2.873(In/Hr) for a 10.0 year storm  
Subarea runoff = 1.081(CFS) for 0.440(Ac.)  
Total runoff = 10.344(CFS) Total area = 4.190(Ac.)

\*\*\*\*\*  
Process from Point/Station 30.000 to Point/Station 40.000  
\*\*\*\* PIPEFLOW TRAVEL TIME (Program estimated size) \*\*\*\*

---

Upstream point/station elevation = 505.900(Ft.)  
Downstream point/station elevation = 502.000(Ft.)  
Pipe length = 150.00(Ft.) Manning's N = 0.013  
No. of pipes = 1 Required pipe flow = 10.344(CFS)  
Nearest computed pipe diameter = 18.00(In.)  
Calculated individual pipe flow = 10.344(CFS)  
Normal flow depth in pipe = 11.06(In.)  
Flow top width inside pipe = 17.52(In.)  
Critical Depth = 14.84(In.)  
Pipe flow velocity = 9.09(Ft/s)  
Travel time through pipe = 0.28 min.  
Time of concentration (TC) = 10.60 min.

\*\*\*\*\*  
Process from Point/Station 50.000 to Point/Station 60.000  
\*\*\*\* INITIAL AREA EVALUATION \*\*\*\*

---

Initial area flow distance = 412.000(Ft.)  
Top (of initial area) elevation = 514.000(Ft.)  
Bottom (of initial area) elevation = 509.500(Ft.)  
Difference in elevation = 4.500(Ft.)  
Slope = 0.01092 s(percent) = 1.09  
TC =  $k(0.300)*[(length^3)/(elevation\ change)]^{0.2}$   
Initial area time of concentration = 8.230 min.  
Rainfall intensity = 3.165(In/Hr) for a 10.0 year storm  
COMMERCIAL subarea type  
Runoff Coefficient = 0.857  
Decimal fraction soil group A = 1.000  
Decimal fraction soil group B = 0.000  
Decimal fraction soil group C = 0.000  
Decimal fraction soil group D = 0.000  
RI index for soil(AMC 2) = 32.00  
Pervious area fraction = 0.100; Impervious fraction = 0.900  
Initial subarea runoff = 1.438(CFS)  
Total initial stream area = 0.530(Ac.)  
Pervious area fraction = 0.100

\*\*\*\*\*  
Process from Point/Station 60.000 to Point/Station 100.000  
\*\*\*\* PIPEFLOW TRAVEL TIME (Program estimated size) \*\*\*\*

---

Upstream point/station elevation = 507.000(Ft.)  
Downstream point/station elevation = 502.000(Ft.)  
Pipe length = 105.00(Ft.) Manning's N = 0.013  
No. of pipes = 1 Required pipe flow = 1.438(CFS)  
Nearest computed pipe diameter = 9.00(In.)  
Calculated individual pipe flow = 1.438(CFS)  
Normal flow depth in pipe = 3.95(In.)  
Flow top width inside pipe = 8.93(In.)  
Critical Depth = 6.63(In.)  
Pipe flow velocity = 7.71(Ft/s)  
Travel time through pipe = 0.23 min.  
Time of concentration (TC) = 8.46 min.

\*\*\*\*\*  
Process from Point/Station 70.000 to Point/Station 80.000  
\*\*\*\* INITIAL AREA EVALUATION \*\*\*\*

---

Initial area flow distance = 150.000(Ft.)  
Top (of initial area) elevation = 509.500(Ft.)  
Bottom (of initial area) elevation = 508.440(Ft.)  
Difference in elevation = 1.060(Ft.)  
Slope = 0.00707 s(percent) = 0.71  
TC =  $k(0.323)*[(length^3)/(elevation\ change)]^{0.2}$   
Initial area time of concentration = 6.454 min.  
Rainfall intensity = 3.645(In/Hr) for a 10.0 year storm  
APARTMENT subarea type  
Runoff Coefficient = 0.821  
Decimal fraction soil group A = 1.000  
Decimal fraction soil group B = 0.000  
Decimal fraction soil group C = 0.000  
Decimal fraction soil group D = 0.000  
RI index for soil(AMC 2) = 32.00  
Pervious area fraction = 0.200; Impervious fraction = 0.800  
Initial subarea runoff = 1.646(CFS)  
Total initial stream area = 0.550(Ac.)  
Pervious area fraction = 0.200

\*\*\*\*\*  
Process from Point/Station 80.000 to Point/Station 90.000  
\*\*\*\* PIPEFLOW TRAVEL TIME (Program estimated size) \*\*\*\*

---

Upstream point/station elevation = 506.000(Ft.)  
Downstream point/station elevation = 505.000(Ft.)  
Pipe length = 170.00(Ft.) Manning's N = 0.013  
No. of pipes = 1 Required pipe flow = 1.646(CFS)  
Nearest computed pipe diameter = 12.00(In.)  
Calculated individual pipe flow = 1.646(CFS)  
Normal flow depth in pipe = 6.71(In.)

Flow top width inside pipe = 11.91(In.)  
Critical Depth = 6.53(In.)  
Pipe flow velocity = 3.64(Ft/s)  
Travel time through pipe = 0.78 min.  
Time of concentration (TC) = 7.23 min.

+++++  
Process from Point/Station 90.000 to Point/Station 90.000  
\*\*\*\* SUBAREA FLOW ADDITION \*\*\*\*

---

APARTMENT subarea type  
Runoff Coefficient = 0.818  
Decimal fraction soil group A = 1.000  
Decimal fraction soil group B = 0.000  
Decimal fraction soil group C = 0.000  
Decimal fraction soil group D = 0.000  
RI index for soil(AMC 2) = 32.00  
Pervious area fraction = 0.200; Impervious fraction = 0.800  
Time of concentration = 7.23 min.  
Rainfall intensity = 3.412(In/Hr) for a 10.0 year storm  
Subarea runoff = 0.279(CFS) for 0.100(Ac.)  
Total runoff = 1.925(CFS) Total area = 0.650(Ac.)

+++++  
Process from Point/Station 90.000 to Point/Station 90.000  
\*\*\*\* SUBAREA FLOW ADDITION \*\*\*\*

---

APARTMENT subarea type  
Runoff Coefficient = 0.818  
Decimal fraction soil group A = 1.000  
Decimal fraction soil group B = 0.000  
Decimal fraction soil group C = 0.000  
Decimal fraction soil group D = 0.000  
RI index for soil(AMC 2) = 32.00  
Pervious area fraction = 0.200; Impervious fraction = 0.800  
Time of concentration = 7.23 min.  
Rainfall intensity = 3.412(In/Hr) for a 10.0 year storm  
Subarea runoff = 0.558(CFS) for 0.200(Ac.)  
Total runoff = 2.484(CFS) Total area = 0.850(Ac.)

+++++  
Process from Point/Station 90.000 to Point/Station 100.000  
\*\*\*\* PIPEFLOW TRAVEL TIME (Program estimated size) \*\*\*\*

---

Upstream point/station elevation = 505.000(Ft.)  
Downstream point/station elevation = 502.000(Ft.)  
Pipe length = 155.00(Ft.) Manning's N = 0.013  
No. of pipes = 1 Required pipe flow = 2.484(CFS)  
Nearest computed pipe diameter = 12.00(In.)  
Calculated individual pipe flow = 2.484(CFS)  
Normal flow depth in pipe = 6.01(In.)  
Flow top width inside pipe = 12.00(In.)  
Critical Depth = 8.10(In.)  
Pipe flow velocity = 6.31(Ft/s)  
Travel time through pipe = 0.41 min.  
Time of concentration (TC) = 7.64 min.  
End of computations, total study area = 5.57 (Ac.)  
The following figures may  
be used for a unit hydrograph study of the same area.

Area averaged pervious area fraction(Ap) = 0.115  
Area averaged RI index number = 32.0

Riverside County Rational Hydrology Program

CIVILCADD/CIVILDESIGN Engineering Software, (c) 1989 - 2014 Version 9.0  
Rational Hydrology Study Date: 04/24/20 File: DAP100YR.out

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DESERT AIDS/CVHC HYDROLOGY CALCULATIONS  
100-YR RATIONAL METHOD ANALYSIS  
PROPOSED CONDITION HYDROLOGY  
BY PKASTURI APRIL 2020  
-----

\*\*\*\*\* Hydrology Study Control Information \*\*\*\*\*

English (in-lb) Units used in input data file  
-----

Program License Serial Number 6388  
-----

Rational Method Hydrology Program based on  
Riverside County Flood Control & Water Conservation District  
1978 hydrology manual

Storm event (year) = 100.00 Antecedent Moisture Condition = 2

Standard intensity-duration curves data (Plate D-4.1)  
For the [ Palm Springs ] area used.  
10 year storm 10 minute intensity = 2.830 (In/Hr)  
10 year storm 60 minute intensity = 1.000 (In/Hr)  
100 year storm 10 minute intensity = 4.520 (In/Hr)  
100 year storm 60 minute intensity = 1.600 (In/Hr)

Storm event year = 100.0  
Calculated rainfall intensity data:  
1 hour intensity = 1.600 (In/Hr)  
Slope of intensity duration curve = 0.5800

+++++  
Process from Point/Station 10.000 to Point/Station 20.000  
\*\*\*\* INITIAL AREA EVALUATION \*\*\*\*

-----  
Initial area flow distance = 575.000 (Ft.)  
Top (of initial area) elevation = 514.240 (Ft.)  
Bottom (of initial area) elevation = 508.660 (Ft.)  
Difference in elevation = 5.580 (Ft.)  
Slope = 0.00970 s (percent) = 0.97  
TC =  $k(0.300)*[(length^3)/(elevation\ change)]^{0.2}$   
Initial area time of concentration = 9.629 min.  
Rainfall intensity = 4.623 (In/Hr) for a 100.0 year storm  
COMMERCIAL subarea type  
Runoff Coefficient = 0.866  
Decimal fraction soil group A = 1.000  
Decimal fraction soil group B = 0.000  
Decimal fraction soil group C = 0.000  
Decimal fraction soil group D = 0.000  
RI index for soil (AMC 2) = 32.00  
Pervious area fraction = 0.100; Impervious fraction = 0.900  
Initial subarea runoff = 11.689 (CFS)  
Total initial stream area = 2.920 (Ac.)  
Pervious area fraction = 0.100

+++++  
Process from Point/Station 20.000 to Point/Station 20.000  
\*\*\*\* SUBAREA FLOW ADDITION \*\*\*\*

-----  
COMMERCIAL subarea type  
Runoff Coefficient = 0.866  
Decimal fraction soil group A = 1.000  
Decimal fraction soil group B = 0.000  
Decimal fraction soil group C = 0.000  
Decimal fraction soil group D = 0.000  
RI index for soil (AMC 2) = 32.00

Pervious area fraction = 0.100; Impervious fraction = 0.900  
Time of concentration = 9.63 min.  
Rainfall intensity = 4.623(In/Hr) for a 100.0 year storm  
Subarea runoff = 1.561(CFS) for 0.390(Ac.)  
Total runoff = 13.250(CFS) Total area = 3.310(Ac.)

+++++  
Process from Point/Station 20.000 to Point/Station 30.000  
\*\*\*\* PIPEFLOW TRAVEL TIME (Program estimated size) \*\*\*\*

---

Upstream point/station elevation = 506.000(Ft.)  
Downstream point/station elevation = 505.000(Ft.)  
Pipe length = 50.00(Ft.) Manning's N = 0.013  
No. of pipes = 1 Required pipe flow = 13.250(CFS)  
Nearest computed pipe diameter = 18.00(In.)  
Calculated individual pipe flow = 13.250(CFS)  
Normal flow depth in pipe = 13.24(In.)  
Flow top width inside pipe = 15.87(In.)  
Critical Depth = 16.31(In.)  
Pipe flow velocity = 9.50(Ft/s)  
Travel time through pipe = 0.09 min.  
Time of concentration (TC) = 9.72 min.

+++++  
Process from Point/Station 30.000 to Point/Station 30.000  
\*\*\*\* SUBAREA FLOW ADDITION \*\*\*\*

---

COMMERCIAL subarea type  
Runoff Coefficient = 0.866  
Decimal fraction soil group A = 1.000  
Decimal fraction soil group B = 0.000  
Decimal fraction soil group C = 0.000  
Decimal fraction soil group D = 0.000  
RI index for soil(AMC 2) = 32.00  
Pervious area fraction = 0.100; Impervious fraction = 0.900  
Time of concentration = 9.72 min.  
Rainfall intensity = 4.599(In/Hr) for a 100.0 year storm  
Subarea runoff = 1.752(CFS) for 0.440(Ac.)  
Total runoff = 15.002(CFS) Total area = 3.750(Ac.)

+++++  
Process from Point/Station 30.000 to Point/Station 30.000  
\*\*\*\* SUBAREA FLOW ADDITION \*\*\*\*

---

COMMERCIAL subarea type  
Runoff Coefficient = 0.866  
Decimal fraction soil group A = 1.000  
Decimal fraction soil group B = 0.000  
Decimal fraction soil group C = 0.000  
Decimal fraction soil group D = 0.000  
RI index for soil(AMC 2) = 32.00  
Pervious area fraction = 0.100; Impervious fraction = 0.900  
Time of concentration = 9.72 min.  
Rainfall intensity = 4.599(In/Hr) for a 100.0 year storm  
Subarea runoff = 1.752(CFS) for 0.440(Ac.)  
Total runoff = 16.753(CFS) Total area = 4.190(Ac.)

+++++  
Process from Point/Station 30.000 to Point/Station 40.000  
\*\*\*\* PIPEFLOW TRAVEL TIME (Program estimated size) \*\*\*\*

---

Upstream point/station elevation = 505.000(Ft.)  
Downstream point/station elevation = 502.000(Ft.)  
Pipe length = 150.00(Ft.) Manning's N = 0.013  
No. of pipes = 1 Required pipe flow = 16.753(CFS)  
Nearest computed pipe diameter = 21.00(In.)  
Calculated individual pipe flow = 16.753(CFS)  
Normal flow depth in pipe = 13.55(In.)  
Flow top width inside pipe = 20.10(In.)  
Critical Depth = 18.03(In.)  
Pipe flow velocity = 10.22(Ft/s)  
Travel time through pipe = 0.24 min.  
Time of concentration (TC) = 9.96 min.



\*\*\*\*\*  
Process from Point/Station 50.000 to Point/Station 60.000  
\*\*\*\* INITIAL AREA EVALUATION \*\*\*\*

---

Initial area flow distance = 412.000(Ft.)  
Top (of initial area) elevation = 514.000(Ft.)  
Bottom (of initial area) elevation = 509.500(Ft.)  
Difference in elevation = 4.500(Ft.)  
Slope = 0.01092 s(percent)= 1.09  
TC = k(0.300)\*[(length^3)/(elevation change)]^0.2  
Initial area time of concentration = 8.230 min.  
Rainfall intensity = 5.064(In/Hr) for a 100.0 year storm  
COMMERCIAL subarea type  
Runoff Coefficient = 0.868  
Decimal fraction soil group A = 1.000  
Decimal fraction soil group B = 0.000  
Decimal fraction soil group C = 0.000  
Decimal fraction soil group D = 0.000  
RI index for soil(AMC 2) = 32.00  
Pervious area fraction = 0.100; Impervious fraction = 0.900  
Initial subarea runoff = 2.329(CFS)  
Total initial stream area = 0.530(Ac.)  
Pervious area fraction = 0.100

\*\*\*\*\*  
Process from Point/Station 60.000 to Point/Station 100.000  
\*\*\*\* PIPEFLOW TRAVEL TIME (Program estimated size) \*\*\*\*

---

Upstream point/station elevation = 507.000(Ft.)  
Downstream point/station elevation = 502.000(Ft.)  
Pipe length = 105.00(Ft.) Manning's N = 0.013  
No. of pipes = 1 Required pipe flow = 2.329(CFS)  
Nearest computed pipe diameter = 9.00(In.)  
Calculated individual pipe flow = 2.329(CFS)  
Normal flow depth in pipe = 5.26(In.)  
Flow top width inside pipe = 8.87(In.)  
Critical Depth = 8.14(In.)  
Pipe flow velocity = 8.69(Ft/s)  
Travel time through pipe = 0.20 min.  
Time of concentration (TC) = 8.43 min.

\*\*\*\*\*  
Process from Point/Station 70.000 to Point/Station 80.000  
\*\*\*\* INITIAL AREA EVALUATION \*\*\*\*

---

Initial area flow distance = 150.000(Ft.)  
Top (of initial area) elevation = 509.500(Ft.)  
Bottom (of initial area) elevation = 508.440(Ft.)  
Difference in elevation = 1.060(Ft.)  
Slope = 0.00707 s(percent)= 0.71  
TC = k(0.323)\*[(length^3)/(elevation change)]^0.2  
Initial area time of concentration = 6.454 min.  
Rainfall intensity = 5.831(In/Hr) for a 100.0 year storm  
APARTMENT subarea type  
Runoff Coefficient = 0.841  
Decimal fraction soil group A = 1.000  
Decimal fraction soil group B = 0.000  
Decimal fraction soil group C = 0.000  
Decimal fraction soil group D = 0.000  
RI index for soil(AMC 2) = 32.00  
Pervious area fraction = 0.200; Impervious fraction = 0.800  
Initial subarea runoff = 2.698(CFS)  
Total initial stream area = 0.550(Ac.)  
Pervious area fraction = 0.200

\*\*\*\*\*  
Process from Point/Station 80.000 to Point/Station 90.000  
\*\*\*\* PIPEFLOW TRAVEL TIME (Program estimated size) \*\*\*\*

---

Upstream point/station elevation = 506.000(Ft.)  
Downstream point/station elevation = 505.000(Ft.)  
Pipe length = 170.00(Ft.) Manning's N = 0.013  
No. of pipes = 1 Required pipe flow = 2.698(CFS)  
Nearest computed pipe diameter = 12.00(In.)  
Calculated individual pipe flow = 2.698(CFS)  
Normal flow depth in pipe = 9.70(In.)

Flow top width inside pipe = 9.44(In.)  
Critical Depth = 8.45(In.)  
Pipe flow velocity = 3.97(Ft/s)  
Travel time through pipe = 0.71 min.  
Time of concentration (TC) = 7.17 min.

+++++  
Process from Point/Station 90.000 to Point/Station 90.000  
\*\*\*\* SUBAREA FLOW ADDITION \*\*\*\*

APARTMENT subarea type  
Runoff Coefficient = 0.839  
Decimal fraction soil group A = 1.000  
Decimal fraction soil group B = 0.000  
Decimal fraction soil group C = 0.000  
Decimal fraction soil group D = 0.000  
RI index for soil(AMC 2) = 32.00  
Pervious area fraction = 0.200; Impervious fraction = 0.800  
Time of concentration = 7.17 min.  
Rainfall intensity = 5.487(In/Hr) for a 100.0 year storm  
Subarea runoff = 0.460(CFS) for 0.100(Ac.)  
Total runoff = 3.158(CFS) Total area = 0.650(Ac.)

+++++  
Process from Point/Station 90.000 to Point/Station 90.000  
\*\*\*\* SUBAREA FLOW ADDITION \*\*\*\*

APARTMENT subarea type  
Runoff Coefficient = 0.839  
Decimal fraction soil group A = 1.000  
Decimal fraction soil group B = 0.000  
Decimal fraction soil group C = 0.000  
Decimal fraction soil group D = 0.000  
RI index for soil(AMC 2) = 32.00  
Pervious area fraction = 0.200; Impervious fraction = 0.800  
Time of concentration = 7.17 min.  
Rainfall intensity = 5.487(In/Hr) for a 100.0 year storm  
Subarea runoff = 0.920(CFS) for 0.200(Ac.)  
Total runoff = 4.078(CFS) Total area = 0.850(Ac.)

+++++  
Process from Point/Station 90.000 to Point/Station 100.000  
\*\*\*\* PIPEFLOW TRAVEL TIME (Program estimated size) \*\*\*\*

Upstream point/station elevation = 505.000(Ft.)  
Downstream point/station elevation = 502.000(Ft.)  
Pipe length = 155.00(Ft.) Manning's N = 0.013  
No. of pipes = 1 Required pipe flow = 4.078(CFS)  
Nearest computed pipe diameter = 12.00(In.)  
Calculated individual pipe flow = 4.078(CFS)  
Normal flow depth in pipe = 8.29(In.)  
Flow top width inside pipe = 11.10(In.)  
Critical Depth = 10.25(In.)  
Pipe flow velocity = 7.05(Ft/s)  
Travel time through pipe = 0.37 min.  
Time of concentration (TC) = 7.53 min.  
End of computations, total study area = 5.57 (Ac.)  
The following figures may  
be used for a unit hydrograph study of the same area.

Area averaged pervious area fraction(Ap) = 0.115  
Area averaged RI index number = 32.0

**EXHIBIT “A”**

**PROPOSED CONDITION  
HYDROLOGY MAP**



# **SYNTHETIC UNIT HYDROGRAPH CALCULATIONS**

	A	B	C	D
1	<b>RCFCD SYNTHETIC UNIT HYDROGRAPH</b>			
2	DATA INPUT SHEET			
3				
4	WORKSHEET PREPARED BY:	PK		
5				
6	PROJECT NAME	DAP AND VISTA SUNRISE II		
7	MICHAEL BAKER INTL. JOB#	175261		
8				
9	CONCENTRATION POINT DESIGNATION	PROPOSED RETENTION BASIN A		
10	AREA DESIGNATION	SUBAREAS A-1 THRU A-7		
11				
12	TRIBUTARY AREAS	ACRES		
13				
14	COMMERCIAL	4.57		
15	PAVING/HARDSCAPE			
16	SF - 1 ACRE			
17	SF - 1/2 ACRE			
18	SF - 1/4 ACRE			
19	MF - CONDOMINIUMS			
20	MF - APARTMENTS			
21	MOBILE HOME PARK			
22	LANDSCAPING			
23	RETENTION BASIN	0.50		
24	GOLF COURSE			
25	MOUNTAINOUS			
26	LOW LOSS RATE (PERCENT)	25%		
27				
28	LENGTH OF WATERCOURSE (L)	640		
29	LENGTH TO POINT OPPOSITE CENTROID (Lca)	273		
30				
31	ELEVATION OF HEADWATER	514.5		
32	ELEVATION OF CONCENTRATION POINT	510.5		
33				
34	AVERAGE MANNINGS 'N' VALUE	0.015		
35				
36	STORM FREQUENCY (YEAR)	100		
37				
38	POINT RAIN			
39	3-HOUR	2.47		
40	6-HOUR	3.26		
41	24-HOUR	5.27		
42				
43	BASIN CHARACTERISTICS:	ELEVATION	AREA	
44		502	2471	
45		503	3084	
46		504	3638	
47		505	4206	
48		506	11927	
49		507	13171	
50		508	14452	
51		509	15686	
52		510	17197	
53		510.5	18107	
54				
55				
56				
57				
58				
59				
60				
61	PERCOLATION RATE (in/hr)	0		
62				
63	DRYWELL DATA			
64	NUMBER USED	0		
65	PERCOLATION RATE (cfs)	0.000000		

**RCFCD SYNTHETIC UNIT HYDROGRAPH METHOD**  
**BASIC DATA CALCULATION FORM**  
 SHORTCUT METHOD

PROJECT: DAP AND VISTA SUNRISE II  
 MBI JOB# 175261  
 BY PK DATE 4/25/2020

**PHYSICAL DATA**

[1] CONCENTRATION POINT		PROPOSED RETENTION BASIN A
[2] AREA DESIGNATION		SUBAREAS A-1 THRU A-7
[3] AREA - ACRES		5.070
[4] L- FEET		640
[5] L- MILES		0.121
[6] La- FEET		273
[7] La- MILES		0.052
[8] ELEVATION OF HEADWATER		514.5
[9] ELEVATION OF CONCENTRATION POINT		510.5
[10] H- FEET		4
[11] S- FEET/MILE		33.0
[12] S <sup>0.5</sup>		5.74
[13] L <sup>0.5</sup> /S <sup>0.5</sup>		0.00109
[14] AVERAGE MANNINGS 'N'		0.015
[15] LAG TIME-HOURS		0.027
[16] LAG TIME-MINUTES		1.62
[17] 25% OF LAG-MINUTES		0.40
[18] 40% OF LAG-MINUTES		0.65
[19] UNIT TIME-MINUTES (25%-40% OF LAG)		5
[24] TOTAL PERCOLATION RATE (cfs)		0.00

**RAINFALL DATA**

[1] SOURCE	
[2] FREQUENCY-YEARS	100
[3] DURATION:	
<b>3-HOURS</b>	
[4] POINT RAIN INCHES (Plate E-5.2)	[7] AVERAGE POINT RAIN INCHES
2.47	2.47
	0.00
	0.00
	0.00
SUM [5]	SUM [7]
5.07	2.47
[16] AREA ADJ FACTOR	1.000
[17] ADJ AVG POINT RAIN	2.47
<b>6-HOURS</b>	
[8] POINT RAIN INCHES (Plate E-5.4)	[10] AVERAGE POINT RAIN INCHES
3.26	3.26
	0.00
	0.00
	0.00
SUM [9]	SUM [11]
5.07	3.26
	1.000
	3.26
<b>24-HOURS</b>	
[12] POINT RAIN INCHES (Plate E-5.6)	[14] AVERAGE POINT RAIN INCHES
5.27	5.27
	0.00
	0.00
	0.00
SUM [13]	SUM [15]
5.07	5.27
	1.000
	5.27

**STORM EVENT SUMMARY**

DURATION		3-HOUR	6-HOUR	24-HOUR
EFFECTIVE RAIN	(in)	1.92	2.15	3.32
FLOOD VOLUME	(cu-ft)	35,252	39,646	61,181
	(acre-ft)	0.81	0.91	1.40
REQUIRED STORAGE	(cu-ft)	34,960	39,318	60,675
	(acre-ft)	0.80	0.90	1.39
PEAK FLOW	(cfs)	11.36	10.15	2.81
MAXIMUM WSEL	(ft)	507.31	507.63	509.07





RCFCD SYNTHETIC UNIT HYDROGRAPH METHOD  
100 YEAR - 3 HOUR STORM EVENT

PROJECT: DAP AND VISTA SUNRISE II  
CONCENTRATION POINT: PROPOSED RETENTION BASIN A

BY: PK DATE 4/25/2020

**EFFECTIVE RAIN CALCULATION FORM**

DRAINAGE AREA-ACRES	5.07	
UNIT TIME-MINUTES	5	
LAG TIME - MINUTES	1.62	
UNIT TIME-PERCENT OF LAG	309.1	
TOTAL ADJUSTED STORM RAIN-INCHES	2.47	
CONSTANT LOSS RATE-in/hr	0.19	
LOW LOSS RATE - PERCENT	25%	
TOTAL PERCOLATION RATE (cfs)		0.00 cfs

Unit Time Period	Time		Pattern Percent (Plate E-5.9)	Storm Rain in/hr	Loss Rate in/hr		Effective Rain in/hr	Flood Hydrograph Flow cfs	Required Storage cf
	Minutes	Hours			Max	Low			
	1	5			0.08	1.3			
2	10	0.17	1.3	0.385	0.19	0.10	0.196	1.00	298.81
3	15	0.25	1.1	0.326	0.19	0.08	0.137	0.70	208.64
4	20	0.33	1.5	0.445	0.19	0.11	0.256	1.30	388.97
5	25	0.42	1.5	0.445	0.19	0.11	0.256	1.30	388.97
6	30	0.50	1.8	0.534	0.19	0.13	0.345	1.75	524.22
7	35	0.58	1.5	0.445	0.19	0.11	0.256	1.30	388.97
8	40	0.67	1.8	0.534	0.19	0.13	0.345	1.75	524.22
9	45	0.75	1.8	0.534	0.19	0.13	0.345	1.75	524.22
10	50	0.83	1.5	0.445	0.19	0.11	0.256	1.30	388.97
11	55	0.92	1.6	0.474	0.19	0.12	0.285	1.45	434.06
12	60	1.00	1.8	0.534	0.19	0.13	0.345	1.75	524.22
13	65	1.08	2.2	0.652	0.19	0.16	0.463	2.35	704.55
14	70	1.17	2.2	0.652	0.19	0.16	0.463	2.35	704.55
15	75	1.25	2.2	0.652	0.19	0.16	0.463	2.35	704.55
16	80	1.33	2.0	0.593	0.19	0.15	0.404	2.05	614.39
17	85	1.42	2.6	0.771	0.19	0.19	0.582	2.95	884.88
18	90	1.50	2.7	0.800	0.19	0.20	0.611	3.10	929.96
19	95	1.58	2.4	0.711	0.19	0.18	0.522	2.65	794.72
20	100	1.67	2.7	0.800	0.19	0.20	0.611	3.10	929.96
21	105	1.75	3.3	0.978	0.19	0.25	0.789	4.00	1200.46
22	110	1.83	3.1	0.919	0.19	0.23	0.730	3.70	1110.29
23	115	1.92	2.9	0.860	0.19	0.22	0.671	3.40	1020.13
24	120	2.00	3.0	0.889	0.19	0.22	0.700	3.55	1065.21
25	125	2.08	3.1	0.919	0.19	0.23	0.730	3.70	1110.29
26	130	2.17	4.2	1.245	0.19	0.31	1.056	5.35	1606.20
27	135	2.25	5.0	1.482	0.19	0.37	1.293	6.56	1966.86
28	140	2.33	3.5	1.037	0.19	0.26	0.849	4.30	1290.62
29	145	2.42	6.8	2.016	0.19	0.51	1.827	9.26	2778.34
30	150	2.50	7.3	2.164	0.19	0.54	1.975	10.01	3003.76
31	155	2.58	8.2	2.430	0.19	0.61	2.242	11.36	3409.50
32	160	2.67	5.9	1.749	0.19	0.44	1.560	7.91	2372.60
33	165	2.75	2.0	0.593	0.19	0.15	0.404	2.05	614.39
34	170	2.83	1.8	0.534	0.19	0.13	0.345	1.75	524.22
35	175	2.92	1.8	0.534	0.19	0.13	0.345	1.75	524.22
36	180	3.00	0.6	0.178	0.19	0.04	0.133	0.68	202.60

22.985

34960.35

**EFFECTIVE RAIN & FLOOD VOLUMES SUMMARY**

EFFECTIVE RAIN (in)	1.92
FLOOD VOLUME (acft)	0.81
FLOOD VOLUME (cuft)	35251.83
REQUIRED STORAGE (acft)	0.80
REQUIRED STORAGE (cuft)	34960.35
PEAK FLOW RATE (cfs)	11.36

RCFCD SYNTHETIC UNIT HYDROGRAPH METHOD 100 YEAR - 6 HOUR STORM EVENT	PROJECT: DAP AND VISTA SUNRISE II CONCENTRATION POINT: PROPOSED RETENTION BASIN A BY: PK DATE: 4/25/2020
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**EFFECTIVE RAIN CALCULATION FORM**

DRAINAGE AREA-ACRES	5.07	TOTAL PERCOLATION RATE (cfs) <span style="float: right;">0.00 cfs</span>
UNIT TIME-MINUTES	5	
LAG TIME - MINUTES	1.62	
UNIT TIME-PERCENT OF LAG	309.1	
TOTAL ADJUSTED STORM RAIN-INCHES	3.26	
CONSTANT LOSS RATE-in/hr	0.189	
LOW LOSS RATE - PERCENT	25%	

Unit Time Period	Time		Pattern Percent <small>(Plate E-5.9)</small>	Storm Rain in/hr	Loss Rate		Effective Rain in/hr	Flood Hydrograph Flow cfs	Required Storage cf
	Minutes	Hours			in/hr				
					Max	Low			
1	5	0.08	0.5	0.196	0.19	0.05	0.01	0.03	10.25
2	10	0.17	0.6	0.235	0.19	0.06	0.05	0.23	69.75
3	15	0.25	0.6	0.235	0.19	0.06	0.05	0.23	69.75
4	20	0.33	0.6	0.235	0.19	0.06	0.05	0.23	69.75
5	25	0.42	0.6	0.235	0.19	0.06	0.05	0.23	69.75
6	30	0.50	0.7	0.274	0.19	0.07	0.08	0.43	129.25
7	35	0.58	0.7	0.274	0.19	0.07	0.08	0.43	129.25
8	40	0.67	0.7	0.274	0.19	0.07	0.08	0.43	129.25
9	45	0.75	0.7	0.274	0.19	0.07	0.08	0.43	129.25
10	50	0.83	0.7	0.274	0.19	0.07	0.08	0.43	129.25
11	55	0.92	0.7	0.274	0.19	0.07	0.08	0.43	129.25
12	60	1.00	0.8	0.313	0.19	0.08	0.12	0.63	188.75
13	65	1.08	0.8	0.313	0.19	0.08	0.12	0.63	188.75
14	70	1.17	0.8	0.313	0.19	0.08	0.12	0.63	188.75
15	75	1.25	0.8	0.313	0.19	0.08	0.12	0.63	188.75
16	80	1.33	0.8	0.313	0.19	0.08	0.12	0.63	188.75
17	85	1.42	0.8	0.313	0.19	0.08	0.12	0.63	188.75
18	90	1.50	0.8	0.313	0.19	0.08	0.12	0.63	188.75
19	95	1.58	0.8	0.313	0.19	0.08	0.12	0.63	188.75
20	100	1.67	0.8	0.313	0.19	0.08	0.12	0.63	188.75
21	105	1.75	0.8	0.313	0.19	0.08	0.12	0.63	188.75
22	110	1.83	0.8	0.313	0.19	0.08	0.12	0.63	188.75
23	115	1.92	0.8	0.313	0.19	0.08	0.12	0.63	188.75
24	120	2.00	0.9	0.352	0.19	0.09	0.16	0.83	248.25
25	125	2.08	0.8	0.313	0.19	0.08	0.12	0.63	188.75
26	130	2.17	0.9	0.352	0.19	0.09	0.16	0.83	248.25
27	135	2.25	0.9	0.352	0.19	0.09	0.16	0.83	248.25
28	140	2.33	0.9	0.352	0.19	0.09	0.16	0.83	248.25
29	145	2.42	0.9	0.352	0.19	0.09	0.16	0.83	248.25
30	150	2.50	0.9	0.352	0.19	0.09	0.16	0.83	248.25
31	155	2.58	0.9	0.352	0.19	0.09	0.16	0.83	248.25
32	160	2.67	0.9	0.352	0.19	0.09	0.16	0.83	248.25
33	165	2.75	1.0	0.391	0.19	0.10	0.20	1.03	307.75
34	170	2.83	1.0	0.391	0.19	0.10	0.20	1.03	307.75
35	175	2.92	1.0	0.391	0.19	0.10	0.20	1.03	307.75
36	180	3.00	1.0	0.391	0.19	0.10	0.20	1.03	307.75
37	185	3.08	1.0	0.391	0.19	0.10	0.20	1.03	307.75
38	190	3.17	1.1	0.430	0.19	0.11	0.24	1.22	367.25
39	195	3.25	1.1	0.430	0.19	0.11	0.24	1.22	367.25
40	200	3.33	1.1	0.430	0.19	0.11	0.24	1.22	367.25
41	205	3.42	1.2	0.469	0.19	0.12	0.28	1.42	426.76
42	210	3.50	1.3	0.509	0.19	0.13	0.32	1.62	486.26
43	215	3.58	1.4	0.548	0.19	0.14	0.36	1.82	545.76
44	220	3.67	1.4	0.548	0.19	0.14	0.36	1.82	545.76
45	225	3.75	1.5	0.587	0.19	0.15	0.40	2.02	605.26
46	230	3.83	1.5	0.587	0.19	0.15	0.40	2.02	605.26
47	235	3.92	1.6	0.626	0.19	0.16	0.44	2.22	664.76
48	240	4.00	1.6	0.626	0.19	0.16	0.44	2.22	664.76
49	245	4.08	1.7	0.665	0.19	0.17	0.48	2.41	724.26
50	250	4.17	1.8	0.704	0.19	0.18	0.52	2.61	783.76
51	255	4.25	1.9	0.743	0.19	0.19	0.55	2.81	843.27
52	260	4.33	2.0	0.782	0.19	0.20	0.59	3.01	902.77
53	265	4.42	2.1	0.822	0.19	0.21	0.63	3.21	962.27
54	270	4.50	2.1	0.822	0.19	0.21	0.63	3.21	962.27
55	275	4.58	2.2	0.861	0.19	0.22	0.67	3.41	1021.77
56	280	4.67	2.3	0.900	0.19	0.23	0.71	3.60	1081.27

RCFCD SYNTHETIC UNIT HYDROGRAPH METHOD 100 YEAR - 6 HOUR STORM EVENT	PROJECT: CONCENTRATION POINT:	DAP AND VISTA SUNRISE II PROPOSED RETENTION BASIN A
	BY: PK	DATE: 4/25/2020

**EFFECTIVE RAIN CALCULATION FORM**

DRAINAGE AREA-ACRES	5.07	TOTAL PERCOLATION RATE (cfs)	0.00 cfs
UNIT TIME-MINUTES	5		
LAG TIME - MINUTES	1.62		
UNIT TIME-PERCENT OF LAG	309.1		
TOTAL ADJUSTED STORM RAIN-INCHES	3.26		
CONSTANT LOSS RATE-in/hr	0.189		
LOW LOSS RATE - PERCENT	25%		

Unit Time Period	Time		Pattern Percent (Plate E-5.9)	Storm Rain in/hr	Loss Rate		Effective Rain in/hr	Flood Hydrograph Flow cfs	Required Storage cf
	Minutes	Hours			in/hr				
					Max	Low			
57	285	4.75	2.4	0.939	0.19	0.24	0.75	3.80	1140.77
58	290	4.83	2.4	0.939	0.19	0.24	0.75	3.80	1140.77
59	295	4.92	2.5	0.978	0.19	0.25	0.79	4.00	1200.28
60	300	5.00	2.6	1.017	0.19	0.26	0.83	4.20	1259.78
61	305	5.08	3.1	1.213	0.19	0.30	1.02	5.19	1557.28
62	310	5.17	3.6	1.408	0.19	0.35	1.22	6.18	1854.79
63	315	5.25	3.9	1.526	0.19	0.38	1.34	6.78	2033.30
64	320	5.33	4.2	1.643	0.19	0.41	1.45	7.37	2211.80
65	325	5.42	4.7	1.839	0.19	0.46	1.65	8.36	2509.31
66	330	5.50	5.6	2.191	0.19	0.55	2.00	10.15	3044.82
67	335	5.58	1.9	0.743	0.19	0.19	0.55	2.81	843.27
68	340	5.67	0.9	0.352	0.19	0.09	0.16	0.83	248.25
69	345	5.75	0.8	0.235	0.19	0.06	0.05	0.23	69.75
70	350	5.83	0.5	0.196	0.19	0.05	0.01	0.03	10.25
71	355	5.92	0.3	0.117	0.19	0.03	0.09	0.45	133.70
72	360	6.00	0.2	0.078	0.19	0.02	0.06	0.30	88.13

EFFECTIVE RAIN & FLOOD VOLUMES SUMMARY	
EFFECTIVE RAIN (in)	2.15
FLOOD VOLUME (acft)	0.91
FLOOD VOLUME (cuft)	39646.27
REQUIRED STORAGE (acft)	0.90
REQUIRED STORAGE (cuft)	39318.46
PEAK FLOW RATE (cfs)	10.15

RCFCD SYNTHETIC UNIT HYDROGRAPH METHOD 100 YEAR - 24 HOUR STORM EVENT	PROJECT: CONCENTRATION POINT: BY: PK DATE: 4/25/2020	DAP AND VISTA SUNRISE II PROPOSED RETENTION BASIN A
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**EFFECTIVE RAIN CALCULATION FORM**

DRAINAGE AREA-ACRES	5.070	CONSTANT LOSS RATE-in/hr	n/a
UNIT TIME-MINUTES	15	VARIABLE LOSS RATE (AVG) in/hr	0.1889
LAG TIME - MINUTES	1.62	MINIMUM LOSS RATE (for var. loss) - in/hr	0.094
UNIT TIME-PERCENT OF LAG	927.4	LOW LOSS RATE - DECIMAL	0.25
TOTAL ADJUSTED STORM RAIN-INCHES	5.27	PERCOLATION RATE (cfs)	0.00

Unit Time Period	Time		Pattern Percent (Plate E-5.9)	Storm Rain in/hr	Loss Rate in/hr		Effective Rain in/hr	Flood Hydrograph Flow cfs	Required Storage cf
	Minutes	Hours			Max	Low			
1	15	0.25	0.2	0.042	0.334	0.011	0.032	0.16	144.09
2	30	0.50	0.3	0.063	0.330	0.016	0.047	0.24	216.13
3	45	0.75	0.3	0.063	0.326	0.016	0.047	0.24	216.13
4	60	1.00	0.4	0.084	0.322	0.021	0.063	0.32	288.18
5	75	1.25	0.3	0.063	0.318	0.016	0.047	0.24	216.13
6	90	1.50	0.3	0.063	0.314	0.016	0.047	0.24	216.13
7	105	1.75	0.3	0.063	0.311	0.016	0.047	0.24	216.13
8	120	2.00	0.4	0.084	0.307	0.021	0.063	0.32	288.18
9	135	2.25	0.4	0.084	0.303	0.021	0.063	0.32	288.18
10	150	2.50	0.4	0.084	0.300	0.021	0.063	0.32	288.18
11	165	2.75	0.5	0.105	0.296	0.026	0.079	0.40	360.22
12	180	3.00	0.5	0.105	0.292	0.026	0.079	0.40	360.22
13	195	3.25	0.5	0.105	0.289	0.026	0.079	0.40	360.22
14	210	3.50	0.5	0.105	0.285	0.026	0.079	0.40	360.22
15	225	3.75	0.5	0.105	0.281	0.026	0.079	0.40	360.22
16	240	4.00	0.6	0.126	0.278	0.032	0.095	0.48	432.27
17	255	4.25	0.6	0.126	0.274	0.032	0.095	0.48	432.27
18	270	4.50	0.7	0.148	0.271	0.037	0.111	0.56	504.31
19	285	4.75	0.7	0.148	0.267	0.037	0.111	0.56	504.31
20	300	5.00	0.8	0.169	0.264	0.042	0.126	0.64	576.35
21	315	5.25	0.6	0.126	0.261	0.032	0.095	0.48	432.27
22	330	5.50	0.7	0.148	0.257	0.037	0.111	0.56	504.31
23	345	5.75	0.8	0.169	0.254	0.042	0.126	0.64	576.35
24	360	6.00	0.8	0.169	0.250	0.042	0.126	0.64	576.35
25	375	6.25	0.9	0.190	0.247	0.048	0.142	0.72	648.40
26	390	6.50	0.9	0.190	0.244	0.048	0.142	0.72	648.40
27	405	6.75	1.0	0.211	0.241	0.053	0.158	0.80	720.44
28	420	7.00	1.0	0.211	0.237	0.053	0.158	0.80	720.44
29	435	7.25	1.0	0.211	0.234	0.053	0.158	0.80	720.44
30	450	7.50	1.1	0.232	0.231	0.058	0.001	0.01	4.65
31	465	7.75	1.2	0.253	0.228	0.063	0.025	0.13	115.29
32	480	8.00	1.3	0.274	0.225	0.069	0.049	0.25	225.80
33	495	8.25	1.5	0.316	0.221	0.079	0.095	0.48	432.39
34	510	8.50	1.5	0.316	0.218	0.079	0.098	0.50	446.47
35	525	8.75	1.6	0.337	0.215	0.085	0.122	0.62	556.62
36	540	9.00	1.7	0.358	0.212	0.090	0.146	0.74	666.65
37	555	9.25	1.9	0.401	0.209	0.101	0.191	0.97	872.73
38	570	9.50	2.0	0.422	0.206	0.106	0.215	1.09	982.51
39	585	9.75	2.1	0.443	0.203	0.111	0.239	1.21	1092.16
40	600	10.00	2.2	0.464	0.200	0.116	0.263	1.34	1201.67
41	615	10.25	1.5	0.316	0.198	0.079	0.119	0.60	541.56
42	630	10.50	1.5	0.316	0.195	0.079	0.122	0.62	554.63
43	645	10.75	2.0	0.422	0.192	0.106	0.230	1.17	1048.51
44	660	11.00	2.0	0.422	0.189	0.106	0.233	1.18	1061.32
45	675	11.25	1.9	0.401	0.186	0.101	0.214	1.09	977.80
46	690	11.50	1.9	0.401	0.183	0.101	0.217	1.10	990.34
47	705	11.75	1.7	0.358	0.181	0.090	0.178	0.90	810.37
48	720	12.00	1.8	0.379	0.178	0.095	0.201	1.02	918.82
49	735	12.25	2.5	0.527	0.175	0.132	0.352	1.78	1604.27
50	750	12.50	2.6	0.548	0.173	0.138	0.375	1.90	1712.45
51	765	12.75	2.8	0.590	0.170	0.148	0.420	2.13	1916.67
52	780	13.00	2.9	0.611	0.168	0.153	0.444	2.25	2024.56
53	795	13.25	3.4	0.717	0.165	0.180	0.552	2.80	2517.06
54	810	13.50	3.4	0.717	0.163	0.180	0.554	2.81	2528.48
55	825	13.75	2.3	0.485	0.160	0.122	0.325	1.65	1481.68
56	840	14.00	2.3	0.485	0.158	0.122	0.327	1.66	1492.80
57	855	14.25	2.7	0.569	0.155	0.143	0.414	2.10	1888.53
58	870	14.50	2.6	0.548	0.153	0.138	0.395	2.00	1803.16
59	885	14.75	2.6	0.548	0.151	0.138	0.398	2.02	1813.82
60	900	15.00	2.5	0.527	0.148	0.132	0.379	1.92	1728.15

RCFCD SYNTHETIC UNIT HYDROGRAPH METHOD 100 YEAR - 24 HOUR STORM EVENT	PROJECT: CONCENTRATION POINT:	DAP AND VISTA SUNRISE II PROPOSED RETENTION BASIN A
	BY: PK	DATE: 4/25/2020

**EFFECTIVE RAIN CALCULATION FORM**

DRAINAGE AREA-ACRES	5.070	CONSTANT LOSS RATE-in/hr	n/a
UNIT TIME-MINUTES	15	VARIABLE LOSS RATE (AVG) in/hr	0.1889
LAG TIME - MINUTES	1.62	MINIMUM LOSS RATE (for var. loss) - in/hr	0.094
UNIT TIME-PERCENT OF LAG	927.4	LOW LOSS RATE - DECIMAL	0.25
TOTAL ADJUSTED STORM RAIN-INCHES	5.27	PERCOLATION RATE (cfs)	0.00175
			0.00

Unit Time Period	Time		Pattern Percent (Plate E-5.9)	Storm Rain in/hr	Loss Rate in/hr		Effective Rain in/hr	Flood Hydrograph Flow cfs	Required Storage cf
	Minutes	Hours			Max	Low			
61	915	15.25	2.4	0.506	0.146	0.127	0.360	1.82	1642.31
62	930	15.50	2.3	0.485	0.144	0.122	0.341	1.73	1556.32
63	945	15.75	1.9	0.401	0.142	0.101	0.259	1.31	1181.60
64	960	16.00	1.9	0.401	0.139	0.101	0.261	1.32	1191.47
65	975	16.25	0.4	0.084	0.137	0.021	0.063	0.32	288.18
66	990	16.50	0.4	0.084	0.135	0.021	0.063	0.32	288.18
67	1005	16.75	0.3	0.063	0.133	0.016	0.047	0.24	216.13
68	1020	17.00	0.3	0.063	0.131	0.016	0.047	0.24	216.13
69	1035	17.25	0.5	0.105	0.129	0.026	0.079	0.40	360.22
70	1050	17.50	0.5	0.105	0.127	0.026	0.079	0.40	360.22
71	1065	17.75	0.5	0.105	0.125	0.026	0.079	0.40	360.22
72	1080	18.00	0.4	0.084	0.123	0.021	0.063	0.32	288.18
73	1095	18.25	0.4	0.084	0.122	0.021	0.063	0.32	288.18
74	1110	18.50	0.4	0.084	0.120	0.021	0.063	0.32	288.18
75	1125	18.75	0.3	0.063	0.118	0.016	0.047	0.24	216.13
76	1140	19.00	0.2	0.042	0.116	0.011	0.032	0.16	144.09
77	1155	19.25	0.3	0.063	0.115	0.016	0.047	0.24	216.13
78	1170	19.50	0.4	0.084	0.113	0.021	0.063	0.32	288.18
79	1185	19.75	0.3	0.063	0.112	0.016	0.047	0.24	216.13
80	1200	20.00	0.2	0.042	0.110	0.011	0.032	0.16	144.09
81	1215	20.25	0.3	0.063	0.109	0.016	0.047	0.24	216.13
82	1230	20.50	0.3	0.063	0.107	0.016	0.047	0.24	216.13
83	1245	20.75	0.3	0.063	0.106	0.016	0.047	0.24	216.13
84	1260	21.00	0.2	0.042	0.105	0.011	0.032	0.16	144.09
85	1275	21.25	0.3	0.063	0.103	0.016	0.047	0.24	216.13
86	1290	21.50	0.2	0.042	0.102	0.011	0.032	0.16	144.09
87	1305	21.75	0.3	0.063	0.101	0.016	0.047	0.24	216.13
88	1320	22.00	0.2	0.042	0.100	0.011	0.032	0.16	144.09
89	1335	22.25	0.3	0.063	0.099	0.016	0.047	0.24	216.13
90	1350	22.50	0.2	0.042	0.098	0.011	0.032	0.16	144.09
91	1365	22.75	0.2	0.042	0.097	0.011	0.032	0.16	144.09
92	1380	23.00	0.2	0.042	0.097	0.011	0.032	0.16	144.09
93	1395	23.25	0.2	0.042	0.096	0.011	0.032	0.16	144.09
94	1410	23.50	0.2	0.042	0.095	0.011	0.032	0.16	144.09
95	1425	23.75	0.2	0.042	0.095	0.011	0.032	0.16	144.09
96	1440	24.00	0.2	0.042	0.095	0.011	0.032	0.16	144.09

**EFFECTIVE RAIN & FLOOD VOLUMES SUMMARY**

EFFECTIVE RAIN (in)	3.32
FLOOD VOLUME (acft)	1.40
FLOOD VOLUME (cuff)	61181.22
REQUIRED STORAGE (acft)	1.39
REQUIRED STORAGE (cuff)	60675.35
PEAK FLOW (cfs)	2.81



PROPOSED RETENTION BASIN A

MBI JOB # 175261

100 YEAR - 3 HOUR STORM EVENT

UNIT PERIOD	TIME (min)	FLOW IN (cfs)	VOLUME IN (cuft)	TOTAL IN BASIN (cuft)	PERC OUT (cuft)	TOTAL IN BASIN (cuft)	BASIN DEPTH (ft)	BALANCE IN BASIN	
								(cuft)	(acre-ft)
1	5	1.00	299	299	0	299	502.11	299	0.01
2	10	1.00	299	598	0	598	502.22	598	0.01
3	15	0.70	209	806	0	806	502.29	806	0.02
4	20	1.30	389	1,195	0	1,195	502.43	1,195	0.03
5	25	1.30	389	1,584	0	1,584	502.57	1,584	0.04
6	30	1.75	524	2,108	0	2,108	502.76	2,108	0.05
7	35	1.30	389	2,497	0	2,497	502.90	2,497	0.06
8	40	1.75	524	3,022	0	3,022	503.07	3,022	0.07
9	45	1.75	524	3,546	0	3,546	503.23	3,546	0.08
10	50	1.30	389	3,935	0	3,935	503.34	3,935	0.09
11	55	1.45	434	4,369	0	4,369	503.47	4,369	0.10
12	60	1.75	524	4,893	0	4,893	503.63	4,893	0.11
13	65	2.35	705	5,598	0	5,598	503.84	5,598	0.13
14	70	2.35	705	6,302	0	6,302	504.04	6,302	0.14
15	75	2.35	705	7,007	0	7,007	504.22	7,007	0.16
16	80	2.05	614	7,621	0	7,621	504.38	7,621	0.17
17	85	2.95	885	8,506	0	8,506	504.60	8,506	0.20
18	90	3.10	930	9,436	0	9,436	504.84	9,436	0.22
19	95	2.65	795	10,231	0	10,231	505.02	10,231	0.23
20	100	3.10	930	11,161	0	11,161	505.14	11,161	0.26
21	105	4.00	1,200	12,361	0	12,361	505.29	12,361	0.28
22	110	3.70	1,110	13,471	0	13,471	505.42	13,471	0.31
23	115	3.40	1,020	14,492	0	14,492	505.55	14,492	0.33
24	120	3.55	1,065	15,557	0	15,557	505.68	15,557	0.36
25	125	3.70	1,110	16,667	0	16,667	505.82	16,667	0.38
26	130	5.35	1,606	18,273	0	18,273	506.01	18,273	0.42
27	135	6.56	1,967	20,240	0	20,240	506.17	20,240	0.46
28	140	4.30	1,291	21,531	0	21,531	506.27	21,531	0.49
29	145	9.26	2,778	24,309	0	24,309	506.49	24,309	0.56
30	150	10.01	3,004	27,313	0	27,313	506.73	27,313	0.63
31	155	11.36	3,409	30,722	0	30,722	507.00	30,722	0.71
32	160	7.91	2,373	33,095	0	33,095	507.18	33,095	0.76
33	165	2.05	614	33,709	0	33,709	507.22	33,709	0.77
34	170	1.75	524	34,234	0	34,234	507.26	34,234	0.79
35	175	1.75	524	34,758	0	34,758	507.30	34,758	0.80
36	180	0.68	203	34,960	0	34,960	507.31	34,960	0.80

PROPOSED RETENTION BASIN A

MBI JOB # 175261

100 YEAR - 6 HOUR STORM EVENT

UNIT PERIOD	TIME (min)	FLOW IN (cfs)	VOLUME IN (cuft)	TOTAL IN BASIN (cuft)	PERC OUT (cuft)	TOTAL IN BASIN (cuft)	BASIN DEPTH (ft)	BALANCE IN BASIN	
								(cuft)	(acre-ft)
1	5	0.03	10	10	0	10	502.00	10	0.00
2	10	0.23	70	80	0	80	502.03	80	0.00
3	15	0.23	70	150	0	150	502.05	150	0.00
4	20	0.23	70	219	0	219	502.08	219	0.01
5	25	0.23	70	289	0	289	502.10	289	0.01
6	30	0.43	129	418	0	418	502.15	418	0.01
7	35	0.43	129	548	0	548	502.20	548	0.01
8	40	0.43	129	677	0	677	502.24	677	0.02
9	45	0.43	129	806	0	806	502.29	806	0.02
10	50	0.43	129	935	0	935	502.34	935	0.02
11	55	0.43	129	1,065	0	1,065	502.38	1,065	0.02
12	60	0.63	189	1,253	0	1,253	502.45	1,253	0.03
13	65	0.63	189	1,442	0	1,442	502.52	1,442	0.03
14	70	0.63	189	1,631	0	1,631	502.59	1,631	0.04
15	75	0.63	189	1,820	0	1,820	502.66	1,820	0.04
16	80	0.63	189	2,008	0	2,008	502.72	2,008	0.05
17	85	0.63	189	2,197	0	2,197	502.79	2,197	0.05
18	90	0.63	189	2,386	0	2,386	502.86	2,386	0.05
19	95	0.63	189	2,575	0	2,575	502.93	2,575	0.06
20	100	0.63	189	2,763	0	2,763	502.99	2,763	0.06
21	105	0.63	189	2,952	0	2,952	503.05	2,952	0.07
22	110	0.63	189	3,141	0	3,141	503.11	3,141	0.07
23	115	0.63	189	3,330	0	3,330	503.16	3,330	0.08
24	120	0.83	248	3,578	0	3,578	503.24	3,578	0.08
25	125	0.63	189	3,767	0	3,767	503.29	3,767	0.09
26	130	0.83	248	4,015	0	4,015	503.37	4,015	0.09
27	135	0.83	248	4,263	0	4,263	503.44	4,263	0.10
28	140	0.83	248	4,511	0	4,511	503.52	4,511	0.10
29	145	0.83	248	4,760	0	4,760	503.59	4,760	0.11
30	150	0.83	248	5,008	0	5,008	503.66	5,008	0.11
31	155	0.83	248	5,256	0	5,256	503.74	5,256	0.12
32	160	0.83	248	5,504	0	5,504	503.81	5,504	0.13
33	165	1.03	308	5,812	0	5,812	503.90	5,812	0.13
34	170	1.03	308	6,120	0	6,120	503.99	6,120	0.14
35	175	1.03	308	6,428	0	6,428	504.07	6,428	0.15
36	180	1.03	308	6,735	0	6,735	504.15	6,735	0.15
37	185	1.03	308	7,043	0	7,043	504.23	7,043	0.16
38	190	1.22	367	7,410	0	7,410	504.32	7,410	0.17
39	195	1.22	367	7,778	0	7,778	504.42	7,778	0.18
40	200	1.22	367	8,145	0	8,145	504.51	8,145	0.19
41	205	1.42	427	8,572	0	8,572	504.62	8,572	0.20
42	210	1.62	486	9,058	0	9,058	504.74	9,058	0.21
43	215	1.82	546	9,604	0	9,604	504.88	9,604	0.22
44	220	1.82	546	10,150	0	10,150	505.01	10,150	0.23
45	225	2.02	605	10,755	0	10,755	505.09	10,755	0.25
46	230	2.02	605	11,360	0	11,360	505.16	11,360	0.26
47	235	2.22	665	12,025	0	12,025	505.24	12,025	0.28
48	240	2.22	665	12,690	0	12,690	505.33	12,690	0.29
49	245	2.41	724	13,414	0	13,414	505.42	13,414	0.31
50	250	2.61	784	14,198	0	14,198	505.51	14,198	0.33
51	255	2.81	843	15,041	0	15,041	505.62	15,041	0.35
52	260	3.01	903	15,944	0	15,944	505.73	15,944	0.37
53	265	3.21	962	16,906	0	16,906	505.85	16,906	0.39
54	270	3.21	962	17,868	0	17,868	505.97	17,868	0.41
55	275	3.41	1,022	18,890	0	18,890	506.06	18,890	0.43



PROPOSED RETENTION BASIN A

MBI JOB # 175261

100 YEAR - 6 HOUR STORM EVENT

UNIT PERIOD	TIME (min)	FLOW IN (cfs)	VOLUME IN (cuft)	TOTAL IN BASIN (cuft)	PERC OUT (cuft)	TOTAL IN BASIN (cuft)	BASIN DEPTH (ft)	BALANCE IN BASIN (cuft) (acre-ft)	
56	280	3.60	1,081	19,971	0	19,971	506.15	19,971	0.46
57	285	3.80	1,141	21,112	0	21,112	506.24	21,112	0.48
58	290	3.80	1,141	22,253	0	22,253	506.33	22,253	0.51
59	295	4.00	1,200	23,453	0	23,453	506.42	23,453	0.54
60	300	4.20	1,260	24,713	0	24,713	506.52	24,713	0.57
61	305	5.19	1,557	26,270	0	26,270	506.65	26,270	0.60
62	310	6.18	1,855	28,125	0	28,125	506.80	28,125	0.65
63	315	6.78	2,033	30,158	0	30,158	506.96	30,158	0.69
64	320	7.37	2,212	32,370	0	32,370	507.12	32,370	0.74
65	325	8.36	2,509	34,879	0	34,879	507.30	34,879	0.80
66	330	10.15	3,045	37,924	0	37,924	507.52	37,924	0.87
67	335	2.81	843	38,767	0	38,767	507.59	38,767	0.89
68	340	0.83	248	39,016	0	39,016	507.60	39,016	0.90
69	345	0.23	70	39,085	0	39,085	507.61	39,085	0.90
70	350	0.03	10	39,096	0	39,096	507.61	39,096	0.90
71	355	0.45	134	39,229	0	39,229	507.62	39,229	0.90
72	360	0.30	89	39,318	0	39,318	507.63	39,318	0.90

PROPOSED RETENTION BASIN A

MBI JOB # 175261

100 YEAR - 24 HOUR STORM EVENT

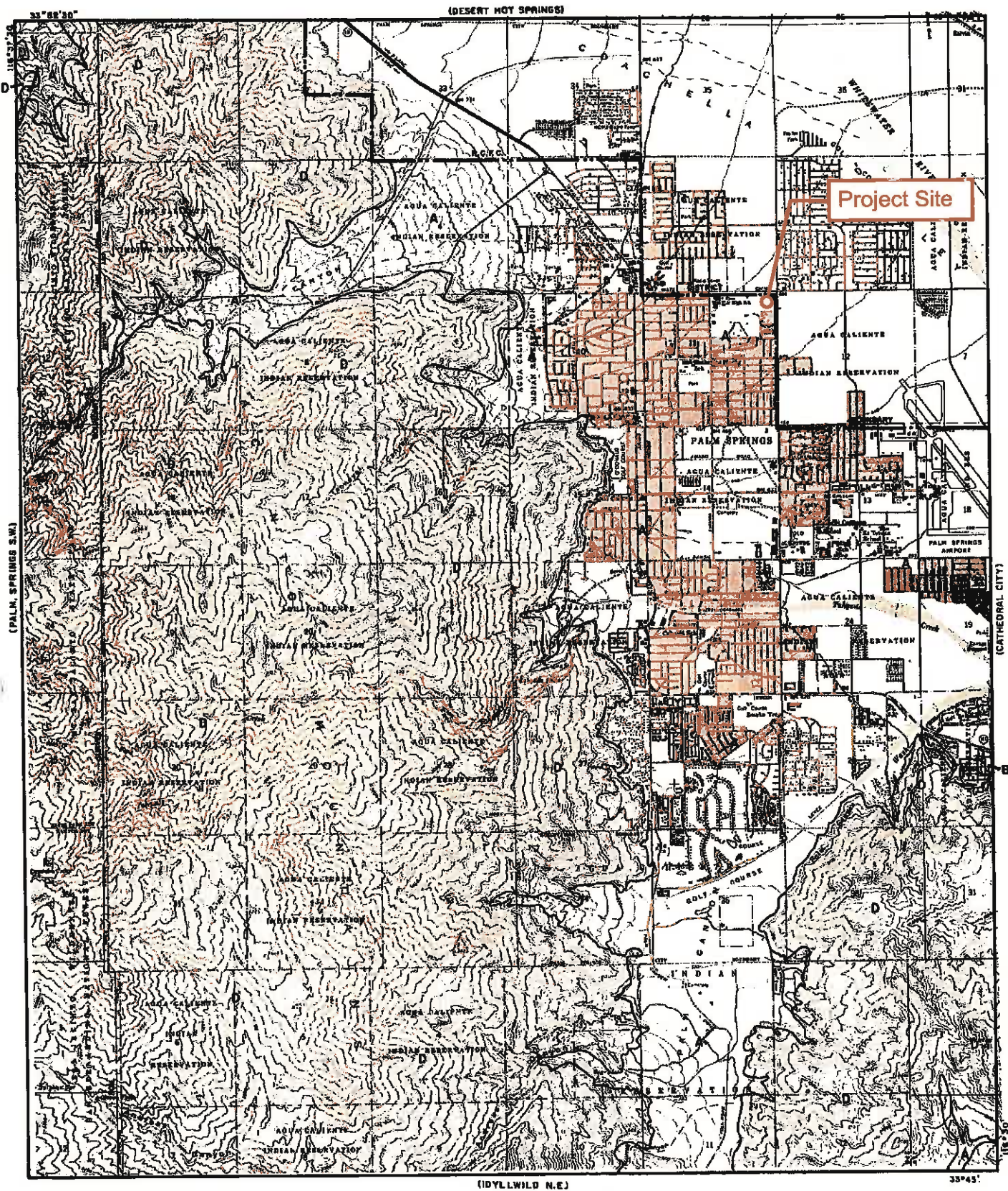
UNIT PERIOD	TIME (min)	FLOW IN (cfs)	VOLUME IN (cuft)	TOTAL IN BASIN (cuft)	PERC OUT (cuft)	TOTAL IN BASIN (cuft)	BASIN DEPTH (ft)	BALANCE IN BASIN	
								(cuft)	(acre-ft)
1	15	0.16	144	144	0	144	502.05	144	0.00
2	30	0.24	216	360	0	360	502.13	360	0.01
3	45	0.24	216	576	0	576	502.21	576	0.01
4	60	0.32	288	865	0	865	502.31	865	0.02
5	75	0.24	216	1,081	0	1,081	502.39	1,081	0.02
6	90	0.24	216	1,297	0	1,297	502.47	1,297	0.03
7	105	0.24	216	1,513	0	1,513	502.54	1,513	0.03
8	120	0.32	288	1,801	0	1,801	502.65	1,801	0.04
9	135	0.32	288	2,089	0	2,089	502.75	2,089	0.05
10	150	0.32	288	2,377	0	2,377	502.86	2,377	0.05
11	165	0.40	360	2,738	0	2,738	502.99	2,738	0.06
12	180	0.40	360	3,098	0	3,098	503.10	3,098	0.07
13	195	0.40	360	3,458	0	3,458	503.20	3,458	0.08
14	210	0.40	360	3,818	0	3,818	503.31	3,818	0.09
15	225	0.40	360	4,179	0	4,179	503.42	4,179	0.10
16	240	0.48	432	4,611	0	4,611	503.55	4,611	0.11
17	255	0.48	432	5,043	0	5,043	503.67	5,043	0.12
18	270	0.56	504	5,547	0	5,547	503.82	5,547	0.13
19	285	0.56	504	6,052	0	6,052	503.97	6,052	0.14
20	300	0.64	576	6,628	0	6,628	504.12	6,628	0.15
21	315	0.48	432	7,060	0	7,060	504.24	7,060	0.16
22	330	0.56	504	7,565	0	7,565	504.36	7,565	0.17
23	345	0.64	576	8,141	0	8,141	504.51	8,141	0.19
24	360	0.64	576	8,717	0	8,717	504.66	8,717	0.20
25	375	0.72	648	9,366	0	9,366	504.82	9,366	0.22
26	390	0.72	648	10,014	0	10,014	504.99	10,014	0.23
27	405	0.80	720	10,735	0	10,735	505.08	10,735	0.25
28	420	0.80	720	11,455	0	11,455	505.17	11,455	0.26
29	435	0.80	720	12,175	0	12,175	505.26	12,175	0.28
30	450	0.01	5	12,180	0	12,180	505.26	12,180	0.28
31	465	0.13	115	12,295	0	12,295	505.28	12,295	0.28
32	480	0.25	226	12,521	0	12,521	505.31	12,521	0.29
33	495	0.48	432	12,954	0	12,954	505.36	12,954	0.30
34	510	0.50	446	13,400	0	13,400	505.41	13,400	0.31
35	525	0.62	557	13,957	0	13,957	505.48	13,957	0.32
36	540	0.74	667	14,623	0	14,623	505.57	14,623	0.34
37	555	0.97	873	15,496	0	15,496	505.67	15,496	0.36
38	570	1.09	983	16,479	0	16,479	505.80	16,479	0.38
39	585	1.21	1,092	17,571	0	17,571	505.93	17,571	0.40
40	600	1.34	1,202	18,772	0	18,772	506.05	18,772	0.43
41	615	0.60	542	19,314	0	19,314	506.09	19,314	0.44
42	630	0.62	555	19,869	0	19,869	506.14	19,869	0.46
43	645	1.17	1,049	20,917	0	20,917	506.22	20,917	0.48
44	660	1.18	1,061	21,978	0	21,978	506.31	21,978	0.50
45	675	1.09	978	22,956	0	22,956	506.38	22,956	0.53
46	690	1.10	990	23,947	0	23,947	506.46	23,947	0.55
47	705	0.90	810	24,757	0	24,757	506.53	24,757	0.57
48	720	1.02	919	25,676	0	25,676	506.60	25,676	0.59
49	735	1.78	1,604	27,280	0	27,280	506.73	27,280	0.63
50	750	1.90	1,712	28,992	0	28,992	506.87	28,992	0.67
51	765	2.13	1,917	30,909	0	30,909	507.02	30,909	0.71
52	780	2.25	2,025	32,934	0	32,934	507.16	32,934	0.76
53	795	2.80	2,517	35,451	0	35,451	507.35	35,451	0.81
54	810	2.81	2,528	37,979	0	37,979	507.53	37,979	0.87
55	825	1.65	1,482	39,461	0	39,461	507.64	39,461	0.91
56	840	1.66	1,493	40,954	0	40,954	507.74	40,954	0.94
57	855	2.10	1,889	42,842	0	42,842	507.88	42,842	0.98
58	870	2.00	1,803	44,645	0	44,645	508.01	44,645	1.02



PROPOSED RETENTION BASIN A  
 MBI JOB # 175261  
 100 YEAR - 24 HOUR STORM EVENT

UNIT PERIOD	TIME (min)	FLOW IN (cfs)	VOLUME IN (cuft)	TOTAL IN BASIN (cuft)	PERC OUT (cuft)	TOTAL IN BASIN (cuft)	BASIN DEPTH (ft)	BALANCE IN BASIN	
								(cuft)	(acre-ft)
59	885	2.02	1,814	46,459	0	46,459	508.13	46,459	1.07
60	900	1.92	1,728	48,187	0	48,187	508.25	48,187	1.11
61	915	1.82	1,642	49,830	0	49,830	508.35	49,830	1.14
62	930	1.73	1,556	51,386	0	51,386	508.46	51,386	1.18
63	945	1.31	1,182	52,568	0	52,568	508.54	52,568	1.21
64	960	1.32	1,191	53,759	0	53,759	508.62	53,759	1.23
65	975	0.32	288	54,047	0	54,047	508.63	54,047	1.24
66	990	0.32	288	54,335	0	54,335	508.65	54,335	1.25
67	1005	0.24	216	54,552	0	54,552	508.67	54,552	1.25
68	1020	0.24	216	54,768	0	54,768	508.68	54,768	1.26
69	1035	0.40	360	55,128	0	55,128	508.71	55,128	1.27
70	1050	0.40	360	55,488	0	55,488	508.73	55,488	1.27
71	1065	0.40	360	55,848	0	55,848	508.75	55,848	1.28
72	1080	0.32	288	56,137	0	56,137	508.77	56,137	1.29
73	1095	0.32	288	56,425	0	56,425	508.79	56,425	1.30
74	1110	0.32	288	56,713	0	56,713	508.81	56,713	1.30
75	1125	0.24	216	56,929	0	56,929	508.83	56,929	1.31
76	1140	0.16	144	57,073	0	57,073	508.84	57,073	1.31
77	1155	0.24	216	57,289	0	57,289	508.85	57,289	1.32
78	1170	0.32	288	57,577	0	57,577	508.87	57,577	1.32
79	1185	0.24	216	57,794	0	57,794	508.88	57,794	1.33
80	1200	0.16	144	57,938	0	57,938	508.89	57,938	1.33
81	1215	0.24	216	58,154	0	58,154	508.91	58,154	1.34
82	1230	0.24	216	58,370	0	58,370	508.92	58,370	1.34
83	1245	0.24	216	58,586	0	58,586	508.94	58,586	1.34
84	1260	0.16	144	58,730	0	58,730	508.95	58,730	1.35
85	1275	0.24	216	58,946	0	58,946	508.96	58,946	1.35
86	1290	0.16	144	59,090	0	59,090	508.97	59,090	1.36
87	1305	0.24	216	59,307	0	59,307	508.98	59,307	1.36
88	1320	0.16	144	59,451	0	59,451	508.99	59,451	1.36
89	1335	0.24	216	59,667	0	59,667	509.01	59,667	1.37
90	1350	0.16	144	59,811	0	59,811	509.02	59,811	1.37
91	1365	0.16	144	59,955	0	59,955	509.02	59,955	1.38
92	1380	0.16	144	60,099	0	60,099	509.03	60,099	1.38
93	1395	0.16	144	60,243	0	60,243	509.04	60,243	1.38
94	1410	0.16	144	60,387	0	60,387	509.05	60,387	1.39
95	1425	0.16	144	60,531	0	60,531	509.06	60,531	1.39
96	1440	0.16	144	60,675	0	60,675	509.07	60,675	1.39

# **APPENDIX A**

## **RCFCD & WCD Plate C1.35 D-5.6 NOAA Precipitation Data**



<p style="text-align: center;"><b>LEGEND</b></p> <p>— SOILS GROUP BOUNDARY  A SOILS GROUP DESIGNATION</p> <p style="text-align: center;"><b>RCFC &amp; WCD</b>  HYDROLOGY MANUAL</p>	<p style="text-align: center;"><b>HYDROLOGIC SOILS GROUP MAP</b>  FOR  <b>PALM SPRINGS</b></p>
 	

ACTUAL IMPERVIOUS COVER

Land Use (1)	Range-Percent	Recommended Value For Average Conditions-Percent(2)
Natural or Agriculture	0 - 10	0
Single Family Residential: (3)		
40,000 S. F. (1 Acre) Lots	10 - 25	20
20,000 S. F. (½ Acre) Lots	30 - 45	40
7,200 - 10,000 S. F. Lots	45 - 55	50
Multiple Family Residential:		
Condominiums	45 - 70	65
Apartments	65 - 90	80
Mobile Home Park	60 - 85	75
Commercial, Downtown Business or Industrial	80 -100	90

**Notes:**

1. Land use should be based on ultimate development of the watershed. Long range master plans for the County and incorporated cities should be reviewed to insure reasonable land use assumptions.
2. Recommended values are based on average conditions which may not apply to a particular study area. The percentage impervious may vary greatly even on comparable sized lots due to differences in dwelling size, improvements, etc. Landscape practices should also be considered as it is common in some areas to use ornamental gravels underlain by impervious plastic materials in place of lawns and shrubs. A field investigation of a study area should always be made, and a review of aerial photos, where available may assist in estimating the percentage of impervious cover in developed areas.
3. For typical horse ranch subdivisions increase impervious area 5 percent over the values recommended in the table above.

**RCFC & WCD**  
HYDROLOGY MANUAL

**IMPERVIOUS COVER  
FOR  
DEVELOPED AREAS**



**NOAA Atlas 14, Volume 6, Version 2**  
**Location name: Palm Springs, California, USA\***  
**Latitude: 33.8429°, Longitude: -116.5293°**  
**Elevation: 507.87 ft\*\***  
 \* source: ESRI Maps  
 \*\* source: USGS



**POINT PRECIPITATION FREQUENCY ESTIMATES**

Sanja Perica, Sarah Dietz, Sarah Heim, Lillian Hiner, Kazungu Maitaria, Deborah Martin, Sandra Pavlovic, Ishani Roy, Carl Trypaluk, Dale Unruh, Fenglin Yan, Michael Yekta, Tan Zhao, Geoffrey Bonnin, Daniel Brewer, Li-Chuan Chen, Tye Parzybok, John Yarchoan

NOAA, National Weather Service, Silver Spring, Maryland

[PF tabular](#) | [PF graphical](#) | [Maps & aerals](#)

**PF tabular**

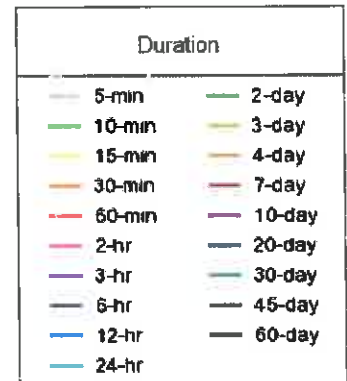
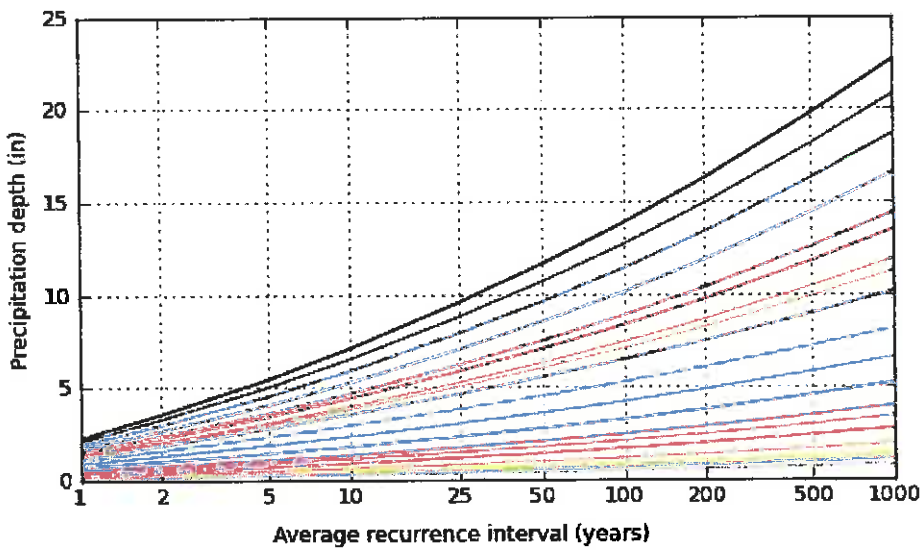
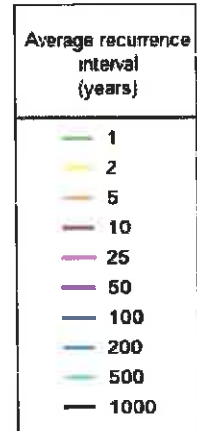
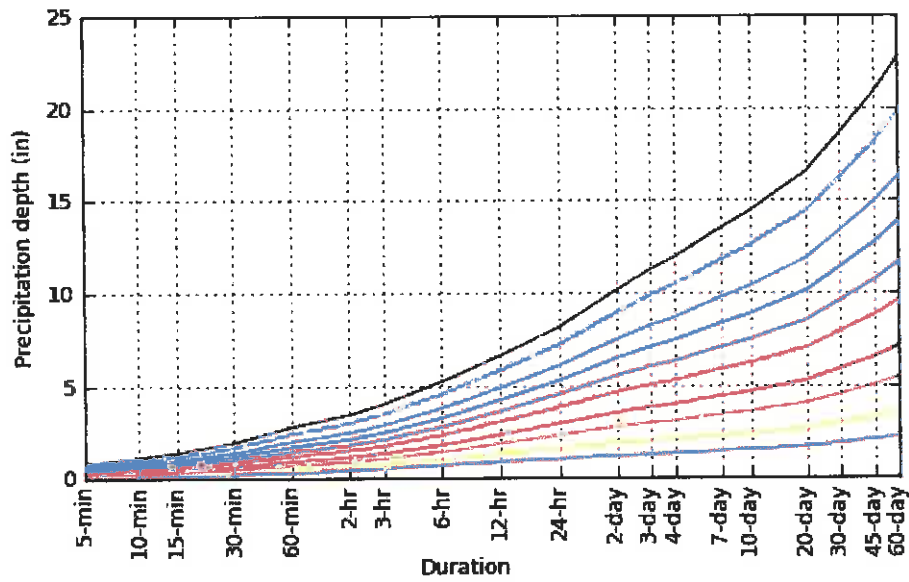
<b>PDS-based point precipitation frequency estimates with 90% confidence intervals (in inches)<sup>1</sup></b>										
Duration	Average recurrence interval (years)									
	1	2	5	10	25	50	100	200	500	1000
<b>5-min</b>	0.090 (0.075-0.109)	0.135 (0.112-0.164)	0.199 (0.165-0.242)	0.255 (0.210-0.313)	0.337 (0.268-0.428)	0.404 (0.315-0.525)	0.478 (0.363-0.636)	0.559 (0.412-0.765)	0.677 (0.479-0.967)	0.777 (0.530-1.15)
<b>10-min</b>	0.128 (0.107-0.156)	0.194 (0.161-0.235)	0.285 (0.237-0.347)	0.365 (0.301-0.449)	0.483 (0.384-0.613)	0.580 (0.451-0.752)	0.685 (0.520-0.912)	0.801 (0.591-1.10)	0.970 (0.686-1.39)	1.11 (0.760-1.65)
<b>15-min</b>	0.155 (0.129-0.188)	0.234 (0.195-0.284)	0.345 (0.286-0.420)	0.442 (0.364-0.542)	0.584 (0.464-0.741)	0.701 (0.546-0.910)	0.829 (0.629-1.10)	0.968 (0.715-1.33)	1.17 (0.830-1.68)	1.35 (0.919-1.99)
<b>30-min</b>	0.221 (0.184-0.268)	0.333 (0.277-0.404)	0.491 (0.407-0.597)	0.628 (0.517-0.771)	0.830 (0.660-1.05)	0.997 (0.776-1.29)	1.18 (0.894-1.57)	1.38 (1.02-1.89)	1.67 (1.18-2.38)	1.91 (1.31-2.83)
<b>60-min</b>	0.318 (0.265-0.386)	0.480 (0.399-0.583)	0.708 (0.587-0.862)	0.906 (0.745-1.11)	1.20 (0.952-1.52)	1.44 (1.12-1.87)	1.70 (1.29-2.26)	1.99 (1.47-2.72)	2.41 (1.70-3.44)	2.76 (1.89-4.08)
<b>2-hr</b>	0.441 (0.368-0.535)	0.633 (0.527-0.769)	0.907 (0.752-1.10)	1.15 (0.944-1.41)	1.50 (1.19-1.91)	1.80 (1.40-2.33)	2.12 (1.61-2.82)	2.47 (1.82-3.38)	2.99 (2.11-4.27)	3.42 (2.34-5.06)
<b>3-hr</b>	0.533 (0.444-0.646)	0.755 (0.628-0.917)	1.07 (0.889-1.30)	1.35 (1.11-1.66)	1.76 (1.40-2.23)	2.10 (1.63-2.72)	2.47 (1.88-3.29)	2.88 (2.13-3.94)	3.48 (2.46-4.97)	3.98 (2.72-5.89)
<b>6-hr</b>	0.718 (0.598-0.870)	1.01 (0.843-1.23)	1.43 (1.19-1.74)	1.80 (1.48-2.21)	2.33 (1.86-2.96)	2.78 (2.16-3.61)	3.26 (2.48-4.34)	3.79 (2.80-5.19)	4.57 (3.23-6.52)	5.21 (3.56-7.71)
<b>12-hr</b>	0.896 (0.746-1.09)	1.30 (1.08-1.58)	1.87 (1.55-2.27)	2.35 (1.93-2.88)	3.05 (2.42-3.87)	3.62 (2.81-4.69)	4.22 (3.21-5.62)	4.88 (3.60-6.68)	5.82 (4.12-8.31)	6.59 (4.50-9.75)
<b>24-hr</b>	1.05 (0.933-1.21)	1.59 (1.40-1.83)	2.32 (2.05-2.68)	2.94 (2.57-3.43)	3.82 (3.24-4.60)	4.53 (3.76-5.56)	5.27 (4.28-6.64)	6.07 (4.79-7.85)	7.21 (5.46-9.70)	8.13 (5.96-11.3)
<b>2-day</b>	1.22 (1.08-1.40)	1.87 (1.66-2.16)	2.78 (2.45-3.21)	3.55 (3.10-4.14)	4.65 (3.94-5.60)	5.54 (4.60-6.81)	6.48 (5.26-8.16)	7.50 (5.92-9.70)	8.95 (6.79-12.1)	10.1 (7.43-14.1)
<b>3-day</b>	1.29 (1.14-1.48)	1.99 (1.76-2.30)	2.98 (2.63-3.45)	3.83 (3.35-4.46)	5.04 (4.27-6.07)	6.03 (5.01-7.42)	7.09 (5.75-8.93)	8.24 (6.50-10.7)	9.89 (7.50-13.3)	11.2 (8.25-15.7)
<b>4-day</b>	1.33 (1.17-1.53)	2.06 (1.82-2.38)	3.09 (2.73-3.58)	3.98 (3.48-4.64)	5.27 (4.46-6.34)	6.32 (5.24-7.76)	7.44 (6.04-9.37)	8.67 (6.84-11.2)	10.4 (7.91-14.0)	11.9 (8.72-16.6)
<b>7-day</b>	1.46 (1.29-1.68)	2.27 (2.01-2.62)	3.42 (3.02-3.96)	4.42 (3.87-5.15)	5.87 (4.97-7.07)	7.06 (5.86-8.68)	8.35 (6.77-10.5)	9.75 (7.69-12.6)	11.8 (8.93-15.9)	13.5 (9.87-18.7)
<b>10-day</b>	1.54 (1.36-1.77)	2.39 (2.11-2.76)	3.61 (3.18-4.17)	4.66 (4.08-5.44)	6.21 (5.26-7.48)	7.49 (6.22-9.20)	8.87 (7.19-11.2)	10.4 (8.19-13.4)	12.6 (9.53-16.9)	14.4 (10.6-20.0)
<b>20-day</b>	1.70 (1.51-1.96)	2.67 (2.36-3.08)	4.05 (3.57-4.69)	5.26 (4.60-6.14)	7.04 (5.97-8.48)	8.52 (7.08-10.5)	10.1 (8.21-12.7)	11.9 (9.37-15.4)	14.4 (10.9-19.4)	16.5 (12.1-23.0)
<b>30-day</b>	1.89 (1.67-2.18)	2.97 (2.63-3.43)	4.52 (3.99-5.23)	5.89 (5.15-6.87)	7.91 (6.70-9.52)	9.58 (7.95-11.8)	11.4 (9.23-14.3)	13.4 (10.6-17.3)	16.2 (12.3-21.9)	18.6 (13.7-25.9)
<b>45-day</b>	2.08 (1.84-2.39)	3.27 (2.89-3.78)	5.00 (4.41-5.78)	6.52 (5.70-7.60)	8.78 (7.44-10.6)	10.7 (8.85-13.1)	12.7 (10.3-16.0)	14.9 (11.8-19.3)	18.1 (13.7-24.4)	20.8 (15.2-28.9)
<b>60-day</b>	2.25 (1.99-2.59)	3.55 (3.13-4.09)	5.43 (4.79-6.28)	7.09 (6.20-8.27)	9.56 (8.10-11.5)	11.6 (9.64-14.3)	13.8 (11.2-17.4)	16.3 (12.8-21.0)	19.8 (15.0-26.6)	22.6 (16.6-31.5)

<sup>1</sup> Precipitation frequency (PF) estimates in this table are based on frequency analysis of partial duration series (PDS). Numbers in parenthesis are PF estimates at lower and upper bounds of the 90% confidence interval. The probability that precipitation frequency estimates (for a given duration and average recurrence interval) will be greater than the upper bound (or less than the lower bound) is 5%. Estimates at upper bounds are not checked against probable maximum precipitation (PMP) estimates and may be higher than currently valid PMP values. Please refer to NOAA Atlas 14 document for more information.

[Back to Top](#)

**PF graphical**

PDS-based depth-duration-frequency (DDF) curves  
 Latitude: 33.8429°, Longitude: -116.5293°



**Maps & aerials**

Small scale terrain





Large scale terrain



Large scale map



Large scale aerial



[Back to Top](#)

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[National Weather Service](#)  
[National Water Center](#)  
1325 East West Highway  
Silver Spring, MD 20910  
Questions?: [HDSC.Questions@noaa.gov](mailto:HDSC.Questions@noaa.gov)

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**NOTICE OF PUBLIC HEARING**  
**RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**  
[www.rcaluc.org](http://www.rcaluc.org)

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893**. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The City of Palm Springs may hold hearings on this item and should be contacted on non-ALUC issues. For more information please contact City of Palm Springs Planner Mr. Glenn Mlaker at (760) 323-8245.

The proposed project application may be viewed at [www.rcaluc.org](http://www.rcaluc.org). Written comments may be submitted to the Riverside County ALUC by e-mail to [prull@rivco.org](mailto:prull@rivco.org) or by U.S. mail to Riverside County Administrative Center, 4080 Lemon Street, 14<sup>th</sup> Floor, Riverside, California 92501. Individuals with disabilities requiring reasonable modifications or accommodations, please telephone Barbara Santos at (951) 955-5132.

**PLACE OF HEARING:** Riverside County Administration Center  
4080 Lemon Street, 1<sup>st</sup> Floor Board Chambers  
Riverside California

**DATE OF HEARING:** August 13, 2020

**TIME OF HEARING:** 9:30 A.M.

**Pursuant to Executive Order N-25-20, this meeting will be conducted by teleconference only. Information on how to participate in the hearing will be available on the ALUC website at [www.rcaluc.org](http://www.rcaluc.org)**

**CASE DESCRIPTION:**

**ZAP1086PS20 – Desert Aids Project (Representative: Terra Nova Planning & Research) –**  
Related Case: City of Palm Springs Case Nos. 5.0934 (PD 281 Amendment, General Plan Amendment), 3.1047 (Major Architectural). The applicant proposes to expand the existing Desert AIDS Project Campus by constructing a new 18,500 square foot medical 'Pavillon' building which includes a lobby, information desk, registration, nurse station, medical administration rooms, conference meeting rooms, consultation offices, and café on 3.89 acres, a 61 unit affordable housing apartment complex on 1.14 acres, and a 17,700 square foot dog park-infiltration basin on 3.09 acres, for a total of 8.12 acres (of a larger existing 13.02 acre site) located on the southwest corner of Vista Chino and Sunrise Way. The applicant also proposes amending the General Plan land use designation of the site from Medium Density Residential and Public/Quasi Public to Mixed Use/Multi-Use. The applicant also proposes to amend the Planned Development District 281, by expanding the project boundary to include all aspects of the project and to allow implementation of the Mixed Use land use designation (Airport Compatibility Zones C and E of the Palm Springs International Airport Influence Area).



# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

PS  
C+E

## APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP 1086 PS 20      DATE SUBMITTED: JUNE 2, 2020

### APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

Applicant	Ms. Sandra Bresson, Mr. David Brinkman	Phone Number	760-323-2118
Mailing Address	Desert AIDS Project 1695 North Sunrise Way Palm Springs, CA 92262	Email	sbresson@desertaidsproject.org
Representative	Nicole Sauviat Criste	Phone Number	760-341-4800
Mailing Address	Terra Nova Planning & Research 42635 Melanie Place, Suite 101 Palm Desert, CA 92211	Email	ncriste@terranovaplanning.com
Property Owner	Desert AIDS Project	Phone Number	760-323-2118
Mailing Address	1695 North Sunrise Way Palm Springs, CA 92262	Email	sbresson@desertaidsproject.org

### LOCAL JURISDICTION AGENCY

Local Agency Name	City of Palm Springs	Phone Number	760+323-8245
Staff Contact	Glenn Mlaker	Email	Glenn.Mlaker@palm Springsca.gov
Mailing Address	3200 East Tahquitz Canyon Wa Palm Springs, CA 92262	Case Type	<input checked="" type="checkbox"/> General Plan / Specific Plan Amendment <input type="checkbox"/> Zoning Ordinance Amendment <input type="checkbox"/> Subdivision Parcel Map / Tentative Tract <input type="checkbox"/> Use Permit <input checked="" type="checkbox"/> Site Plan Review/Plot Plan <input type="checkbox"/> Other
Local Agency Project No	Case No. 5.0934 <b>PD 281 GPA, AMENDMENT 3.1043 MAS</b>		

### PROJECT LOCATION

*Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways*

Street Address	1695 North Sunrise Way Palm Springs, CA 92262	Gross Parcel Size	13.2 acres
Assessor's Parcel No.	507-100-026, -041, -042, -044, -045	Nearest Airport and distance from Airport	PSP, 3200 ft from end of runway
Subdivision Name	N/A		
Lot Number	N/A		

### PROJECT DESCRIPTION

*If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed*

Existing Land Use (describe)	The Desert AIDS Project campus currently consists of 44,000± square feet of medical clinic and office space, an 80 unit affordable housing project (Vista Sunrise Apartments) and an 8,000 square foot office building. A vacant 1.7 acre parcel occurs between the medical clinic and office building.
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Proposed Land Use (describe)	The proposed project includes a General Plan Amendment to Mixed Use/Multi-Use, an amendment to the existing Planned Development District #281, and a Major Architectural Review. The project will include the addition of an 18,500 square foot pavilion in the center of, and linking the existing medical clinic and offices, and 61 units of special needs housing on the vacant 1.7 acre parcel.		
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)	1 parcel, 61 units	
For Other Land Uses (See Appendix C)	Hours of Operation		
	Number of People on Site	Maximum Number	630
	Method of Calculation	See Table 1, attached.	
Height Data	Site Elevation (above mean sea level)	Medical: 512', Apartments: 510'	ft.
	Height of buildings or structures (from the ground)	Medical: 18', Apartments: 33'	ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?	<input type="checkbox"/> Yes	
	If yes, describe	<input checked="" type="checkbox"/> No	

- A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:**
1. . . . . Completed ALUC Application Form
  1. . . . . ALUC fee payment
  1. . . . . Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
  1. . . . . Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
  1. . . . . CD with digital files of the plans (pdf)
  1. . . . . Vicinity Map (8.5x11)
  1. . . . . Detailed project description
  1. . . . . Local jurisdiction project transmittal
  3. . . . . Gummed address labels for applicant/representative/property owner/local jurisdiction planner
  3. . . . . Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. **(Only required if the project is scheduled for a public hearing Commission meeting)**

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# COUNTY OF RIVERSIDE AIRPORT LAND USE COMMISSION

## STAFF REPORT

**AGENDA ITEM:** 3.6

**HEARING DATE:** August 13, 2020

**CASE NUMBER:** ZAP1044TH18 – Empire Airport, LLC/Hagen Company, LLC (Representative: The Altum Group)

**APPROVING JURISDICTION:** City of Coachella

**JURISDICTION CASE NO:** GPA20-01 (General Plan Amendment), CZ20-01 (Change of Zone), CUP324 CUP325 CUP326 (Conditional Use Permits), AR20-04 (Architectural Review), PM39721 (Tentative Parcel Map)

**LAND USE PLAN:** 2005 Jacqueline Cochran Regional Airport Land Use Compatibility Plan (last amended in 2006)

**Airport Influence Area:** Jacqueline Cochran Regional Airport

**Land Use Policy:** Airport Compatibility Zone D

**Noise Levels:** Below 55 CNEL from aircraft at this location

**MAJOR ISSUES:** None

**RECOMMENDATION:** Staff recommends that the Commission find the proposed General Plan Amendment and Change of Zone CONSISTENT with the 2005 Jacqueline Cochran Regional Airport Land Use Compatibility Plan as amended in 2006, and find the proposed Conditional Use Permits, Architectural Review, and Tentative Parcel Map CONSISTENT, subject to the conditions included herein.

**PROJECT DESCRIPTION:** The applicant proposes to develop a 629,450 square foot mixed-use business park center on 42.36 acres including 233,100 square feet of large warehouse buildings, 96,000 square feet of small warehouse manufacturing buildings, 81,000 square feet of small business manufacturing buildings, 133,900 square foot self-storage facility, 76,800 square foot personal vehicle storage garage, a 4,000 square foot gas station convenience store with 10 fueling stations, and a 4,650 square foot restaurant with 9 car stack drive-thru. The applicant also proposes amending the site's General Plan land use designation from Sub-Area 8 into Sub-Area 7, to allow for commercial cannabis-related uses, and changing the site's zoning from Heavy Industrial (M-H) to Manufacturing Service (M-S). A tentative parcel map is also proposed to divide the site into 7 commercial parcels.

**PROJECT LOCATION:** The project site is located northerly of Airport Boulevard, southerly of 55<sup>th</sup> Avenue, westerly of State Route 86, and easterly of the Coachella Valley Water District Storm Water Channel, in the City of Coachella, approximately 6,000 feet northeasterly of the northerly terminus of Runway 17-35 at Jacqueline Cochran Regional Airport.

**BACKGROUND:**

Non-Residential Average Intensity: Compatibility Zone D limits average intensity to 100 people per acre.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, the following rates were used to calculate the occupancy for the proposed buildings:

- Warehouse – 1 person per 500 square feet,
- Manufacturing – 1 person per 200 square feet,
- Office – 1 person per 200 square feet,
- Garage – 1 person per 200 square feet,
- Storage – 1 person per 300 square feet,
- Retail – 1 person per 60 square feet,
- Restaurant dining – 1 person per 15 square feet, and
- Restaurant kitchen – 1 person per 200 square feet.

The project proposes a 629,450 square foot mixed-use business park center on 42.36 acres, including 233,100 square feet of large warehouse buildings (221,445 square feet of warehouse area and 11,655 square feet of office area), 96,000 square feet of small warehouse manufacturing buildings, 81,000 square feet of small business manufacturing buildings, 133,900 square foot self-storage facility, 76,800 square foot personal vehicle storage garage, a 4,000 square foot gas station convenience store (2,000 square feet of retail area, 300 square feet of office area, 1,700 square feet of storage area) with 10 fueling stations, and a 4,650 square foot restaurant building with 9 car stack drive-thru (2,000 square feet of dining area and 2,650 square feet of kitchen area) which would accommodate 1,985 people, resulting in an average intensity of 57 people per acre, which is consistent with the Compatibility Zone D criterion of 100.

If we were to consider the average intensity on a lot-by-lot basis (based on the applicant's proposed lot lines as indicated on the parcel map and site plan):

- Parcel 1 proposes 233,100 square feet of large warehouse area (221,445 square feet of warehouse area and 11,655 square feet of office area) on 9.22 acres would accommodate 501 people, resulting in an average intensity of 54 people per acre,
- Parcel 2 proposes 96,000 square feet of small warehouse manufacturing area on 5.93 acres would accommodate 480 people, resulting in an average intensity of 81 people per acre,

- Parcel 3 proposes a 133,900 square foot self-storage facility on 6.71 acres would accommodate 446 people, resulting in an average intensity of 67 people per acre,
- Parcel 4 proposes 76,800 square feet of personal vehicle storage garage on 4.61 acres would accommodate 384 people, resulting in an average intensity of 83 people per acre,
- Parcel 5 proposes 27,000 square feet of small business manufacturing area on 2.84 acres would accommodate 135 people, resulting in an average intensity of 48 people per acre,
- Parcel 6 proposes 54,000 square feet of small business manufacturing on 3.78 acres would accommodate 270 people, resulting in an average intensity of 71 people per acre, and
- Parcel 7 proposes a 4,000 square foot gas station convenience store (2,000 square feet of retail area, 300 square feet of office area, 1,700 square feet of storage area) with 10 fueling stations, and a 4,650 square foot restaurant building (2,000 square feet of dining area and 2,650 square feet of kitchen area) with 9 car stack drive-thru, on 2.19 acres would accommodate 216 people, resulting in an average intensity of 99 people per acre.

All lots are consistent with the Zone D average intensity criterion of 100 people per acre.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle in the absence of more precise data). Based on the number of parking spaces provided of 687, the total occupancy would be 1,031 people for an average intensity of 24 persons per acre, which would also be consistent with the Zone D average intensity criterion of 100 people per acre.

Any future intensive use within these buildings other than what has been reviewed would require subsequent compatibility evaluation.

Non-Residential Single-Acre Intensity: Compatibility Zone D limits single-acre intensity to 300 people.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre areas in Zone D would be as follows:

- Parcel 1 single-acre area includes 41,120 square feet of warehouse area, and 2,440 square feet of office area, accommodating 94 people,
- Parcel 2 includes 33,600 square feet of small warehouse manufacturing area, accommodating 168 people,



- Parcel 3 includes 29,400 square feet of self-storage facility, accommodating 98 people,
- Parcel 4 includes 20,600 square feet of personal vehicle storage garage, accommodating 103 people,
- Parcel 5 includes 15,500 square feet of small business manufacturing area, accommodating 78 people,
- Parcel 6 includes 21,900 square feet of small business manufacturing area, accommodating 110 people, and
- Parcel 7 includes a 2,000 square foot portion of the gas station convenience store (2,000 square feet of retail area) with 10 fueling stations, and a 4,650 square foot restaurant building (2,000 square feet of dining area and 2,650 square feet of kitchen area) with 9 car stack drive-thru, accommodating 208 people.

All single-acre areas in each lot are consistent with the Compatibility Zone D single acre intensity criterion of 300 people per acre.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in D (highly noise-sensitive outdoor non-residential uses, and hazards to flight) within the project.

Noise: The property lies outside the areas subject to average exterior noise levels greater than 55 CNEL. Therefore, no special measures to mitigate aircraft-generated noise are required.

Part 77: The elevation of Runway 17-35 at its northerly terminus is approximately -114 feet mean sea level (MSL). At a distance of approximately 6,000 feet from the runway to the project site, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding -54 feet mean sea level (MSL). The site elevation is approximately -117 feet MSL, and their maximum building height is 50 feet, for a top point elevation of -67 feet MSL. Therefore, review of buildings by the FAA Obstruction Evaluation Service (FAA OES) is not required.

Open Area: Zone D requires a minimum of 10% of open area for projects 10 acres or larger be set aside as open area that could potentially serve as emergency landing areas. As a 42.36-acre site in Zone D, the project is required to provide a minimum of 4.23 acre of open area consistent with ALUC open area criteria. The applicant has provided 4.32 acres of open area located within the drive aisles and parking areas of the project as shown on the open space exhibit. These areas are to remain as open space and the project is conditioned to maintain at least 4.23 acres of this area consistent with ALUC open area requirements of 300 feet by 75 feet minimum shape, and prohibit obstructions greater than 4 feet in height that are at least 4 inches in diameter.

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA

strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33C). The project is located 6,000 feet from the runway, and therefore would be subject to the above requirement.

The project includes approximately 3.3 acres of temporary infiltration basin area, which will be used on an interim basis as the project will be allowed to drain directly into the adjacent Coachella Storm Channel as soon as approved Coachella Valley Water District channel improvements are completed.

Infiltration basins are not recommended in the vicinity of airports due to the potential that such areas could provide food, water, and shelter for hazardous wildlife. Pursuant to the study “Wildlife Hazard Management at Riverside County Airports: Background and Policy”, October 2018, by Mead & Hunt, which is the basis of the brochure titled “Airports, Wildlife and Stormwater Management”, such basins are to be avoided in Zones D and E, unless they provide for a 48-hour drawdown and propose landscaping that is not attractive to hazardous wildlife.

The applicant has agreed to the following conditions, in order to reduce bird attractant: 1) new basins are to be designed so as to provide for a maximum 48-hour detention period following the conclusion of a storm event, and to remain totally dry between rainfalls, and 2) any landscaping proposed in the detention basin shall be in accordance with the ALUC “Landscaping Near Airports” and “Airports, Wildlife and Stormwater Management” brochures.

General Plan Amendment/Change of Zone: The applicant proposes to amend the site’s General Plan land use designation from Sub-Area 8 into Sub-Area 7 to allow for commercial cannabis related uses, and changing the site’s zoning from Heavy Industrial (M-H) to Manufacturing Service (M-S). The proposed M-S zone does allow retail and restaurant type uses, which could potentially exceed the Compatibility Zone D intensity criteria. The project has been reviewed with a specific occupancy and intensity calculation. A condition is recommended specifying any increases to building area, changes in building use, or changes to the parcel areas, would require subsequent ALUC review.

The proposed amendments would be as, or more, consistent with the Compatibility Plan as long as the underlying development’s intensity is consistent with the compatibility criteria.

Tentative Tract Map: The applicant proposes a tentative parcel map to divide the site’s 42.36 acres into seven commercial parcels. The proposed parcel map would not result in a significant impact to airport land use compatibility, as long as the underlying development’s intensity is consistent with the compatibility criteria.

#### **CONDITIONS:**

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of

lumens or reflection into the sky.

2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations, or any type of strobe light, toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, livestock operations, production of cereal grains, sunflower, and row crops, artificial marshes, wastewater management facilities, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Highly noise-sensitive outdoor non-residential uses and hazards to flight.
3. The attached notice shall be provided to all prospective purchasers of the property and tenants of the buildings thereon, and shall be recorded as a deed notice prior to or in conjunction with recordation of the final parcel map. In the event that the Office of Riverside County Assessor-Clerk-Recorder declines to record said notice, the text of the notice shall be included in the Environmental Constraint Sheet (ECS) of the final parcel map, if an ECS is otherwise required.
4. The new detention basin(s) shall be designed so as to provide a maximum 48-hour detention period for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basin that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the detention basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS,

WILDLIFE AND STORMWATER MANAGEMENT” brochure available at [RCALUC.ORG](http://RCALUC.ORG) which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist. The infiltration basin shall be designed in accordance with all parameters identified in the Wildlife Hazard Management at Riverside County Airports: Background and Policy.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: “There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes”. The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

5. This project has been evaluated as consisting of 233,100 square feet of large warehouse buildings (221,445 square feet of warehouse area and 11,655 square feet of office area), 96,000 square feet of small warehouse manufacturing buildings, 81,000 square feet of small business manufacturing buildings, 133,900 square foot self-storage facility, 76,800 square foot personal vehicle storage garage, a 4,000 square foot gas station convenience store (2,000 square feet of retail area, 300 square feet of office area, 1,700 square feet of storage area) with 10 fueling stations, and a 4,650 square foot restaurant building with 9 car stack drive-thru (2,000 square feet of dining area and 2,650 square feet of kitchen area). Any increase in building area, change in use to any use, or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria.
6. At least 4.23 acres of ALUC-eligible open areas (at least 75 feet in width and 300 feet in length), as depicted on the open space exhibit shall be kept obstacle and obstruction free per ALUC open area definition (no objects greater than four feet in height with a diameter of four inches or greater).

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

# NOTICE

**THERE IS AN AIRPORT NEARBY.  
THIS STORM WATER BASIN IS DESIGNED TO HOLD  
STORM WATER FOR ONLY 48 HOURS AND  
NOT TO ATTRACT BIRDS**

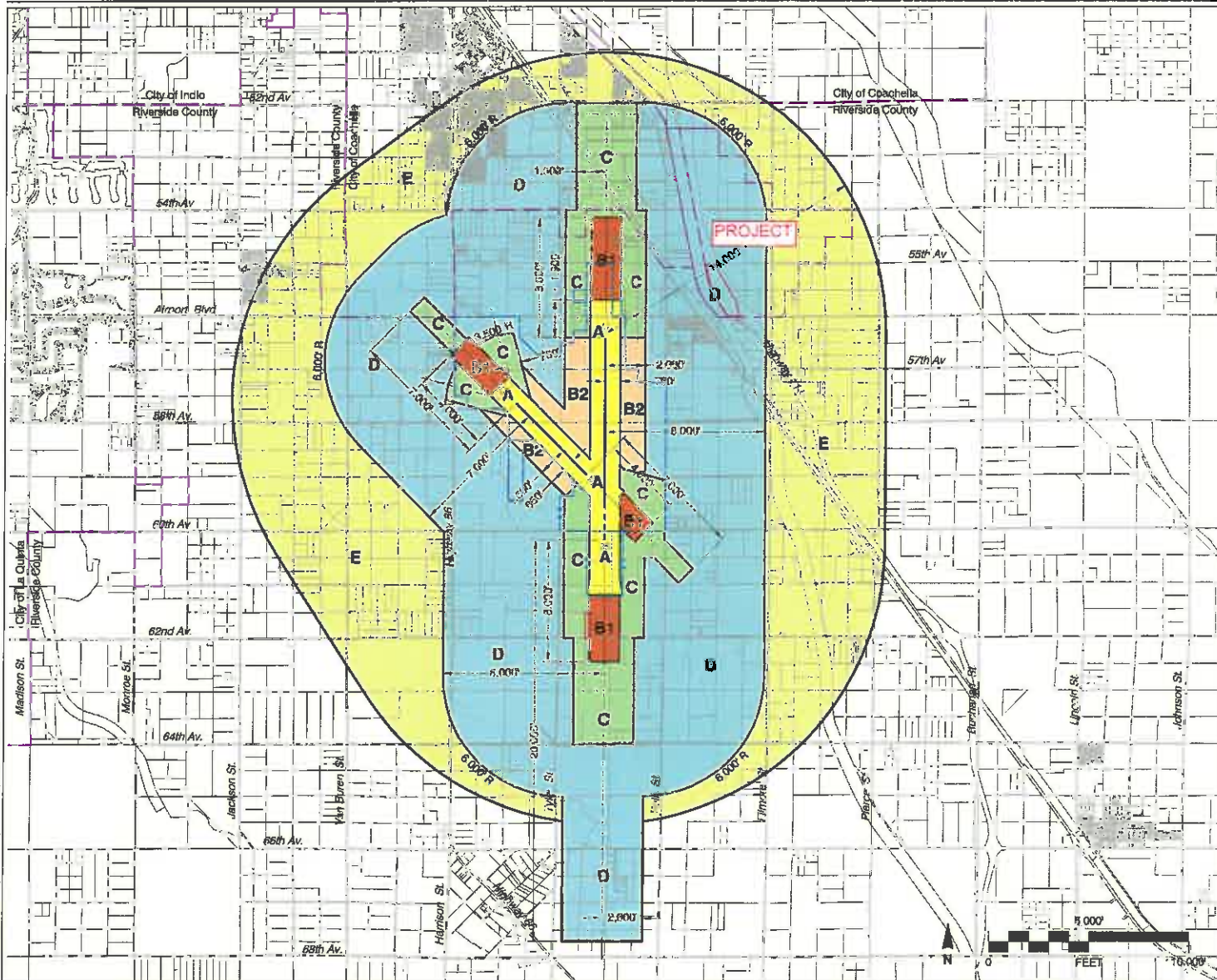
**PROPER MAINTENANCE IS NECESSARY TO AVOID  
BIRD STRIKES**



**IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:**

**Name:** \_\_\_\_\_

**Phone:** \_\_\_\_\_



**Legend**

**Compatibility Zones**

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E

**Boundary Lines**

- Airport Property Line - Existing
- Airport Property Line - Planned
- City Limits

**Note**

Except for southern extension, Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

See Chapter 2, Table 2A for compatibility criteria associated with this map.

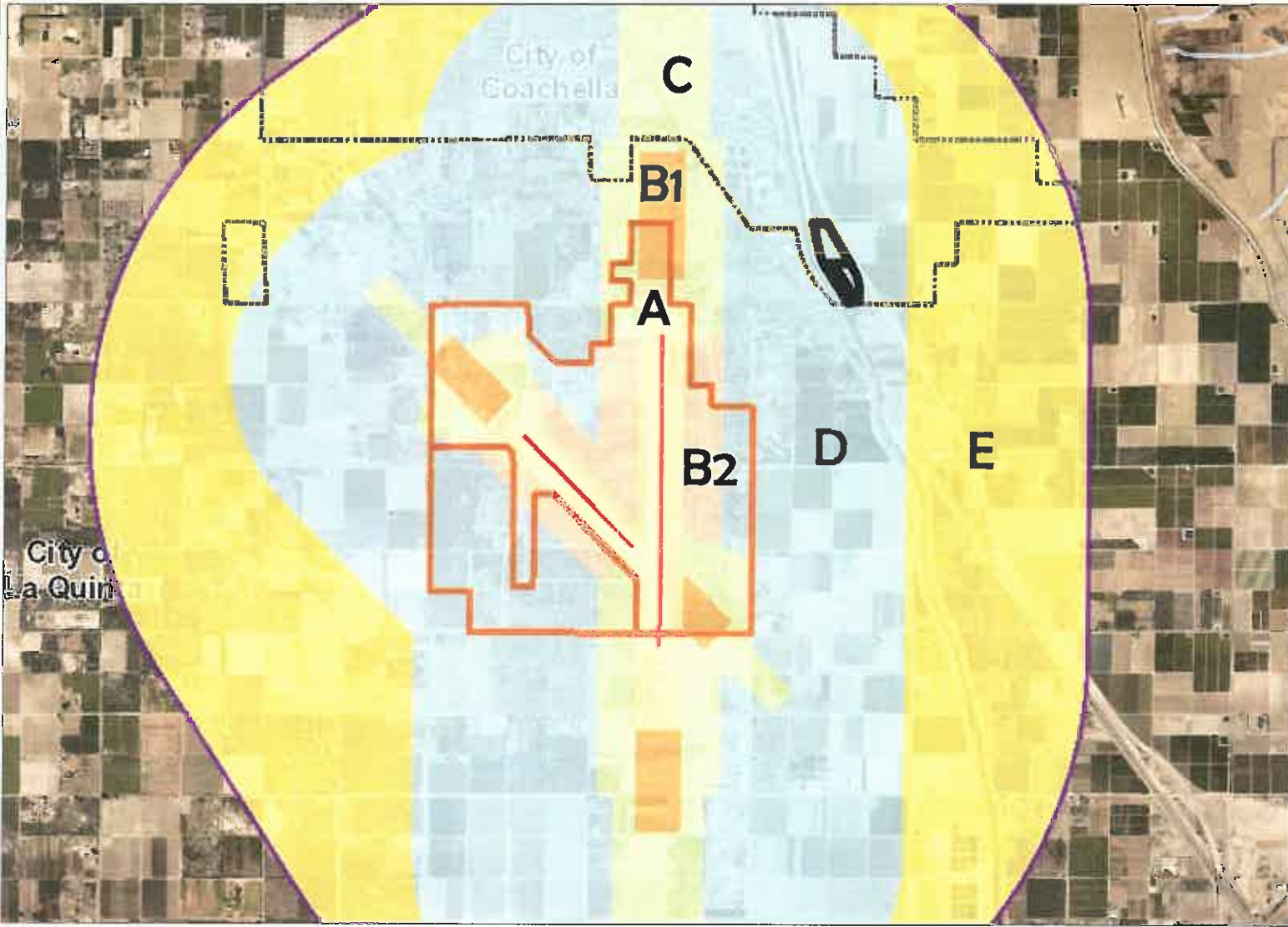
Riverside County  
 Airport Land Use Commission  
 Riverside County  
 Airport Land Use Compatibility Plan  
 Policy Document  
 (Adopted June 2005)

Map JC-1

**Compatibility Map**  
 Jacqueline Cochran Regional Airport

3.0000, ESTIMATED, Jun 05, 10:11:11 AM

# Map My County Map



**Legend**

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC8

0 6 12,037 Feet

**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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**Notes**



# Map My County Map



### Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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### Notes

# Map My County Map



### Legend

- Blue line Streams
- City Areas
- World Street Map

### Notes



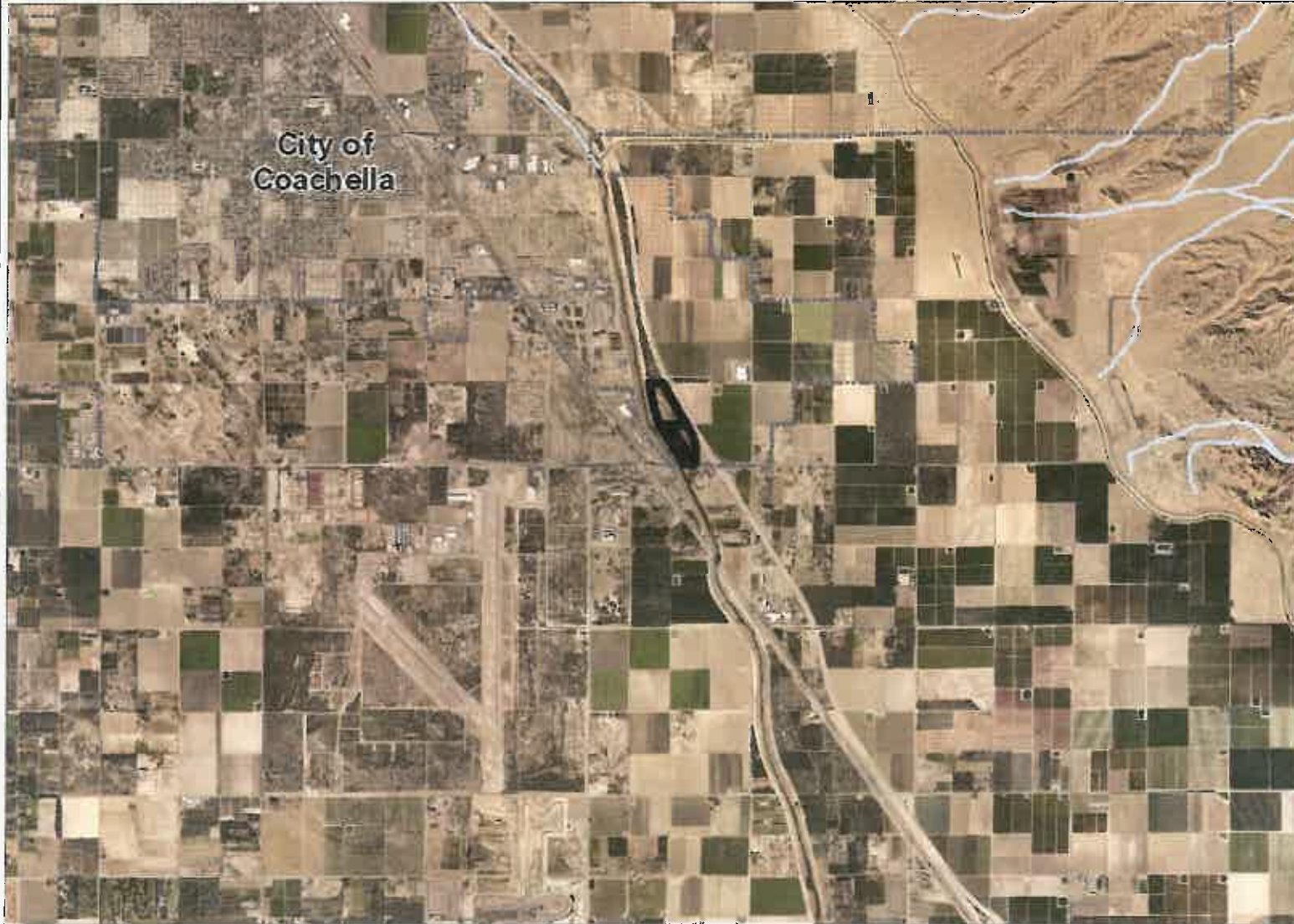
**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.



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# Map My County Map



## Legend

- Blueline Streams
- City Areas
- World Street Map



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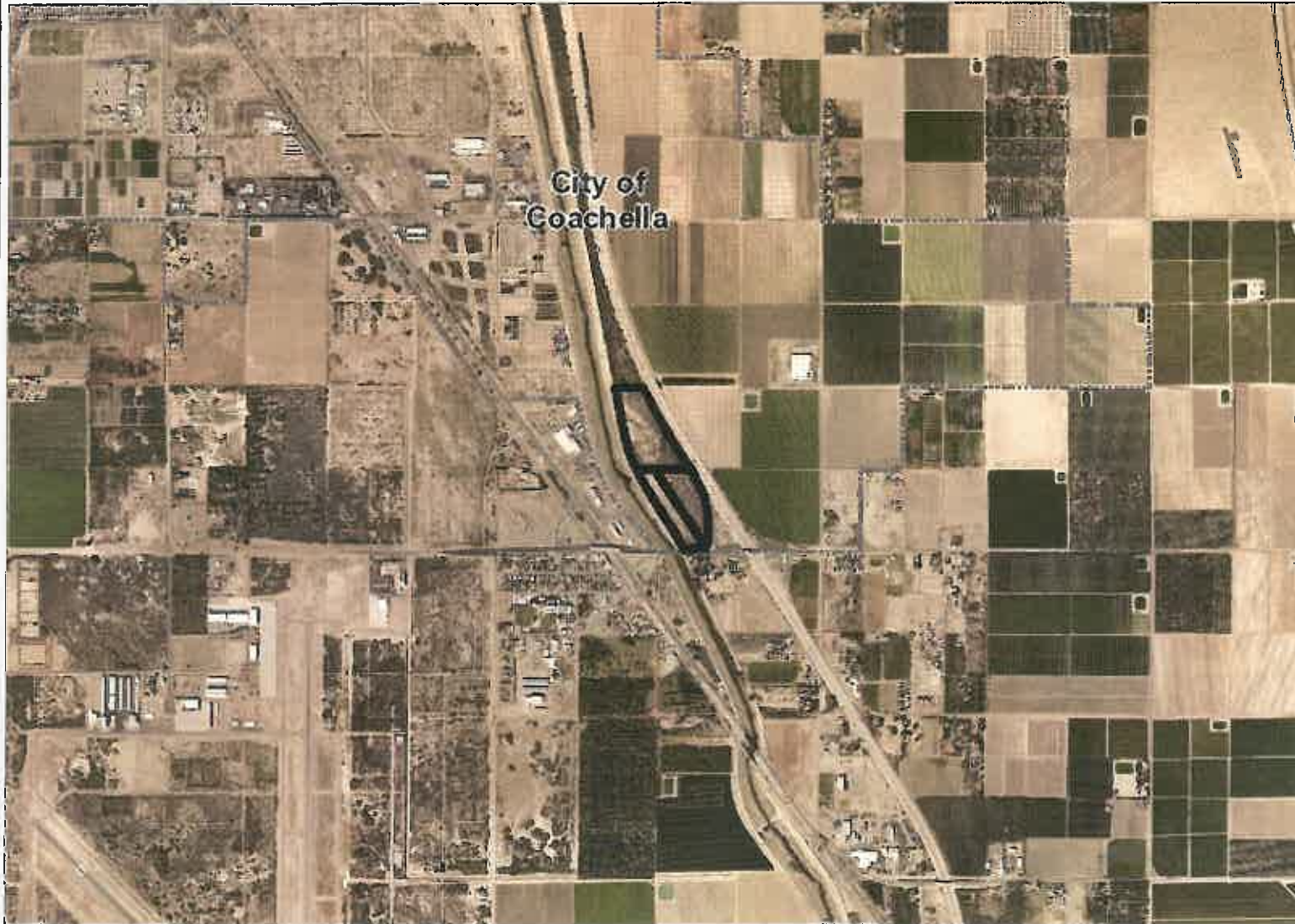


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## Notes

# Map My County Map



## Legend

- Blueline Streams
- City Areas
- World Street Map



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## Notes

# Map My County Map



## Legend

-  Parcels
-  Blueline Streams
-  City Areas
-  World Street Map



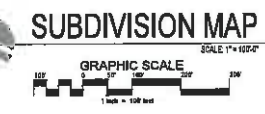
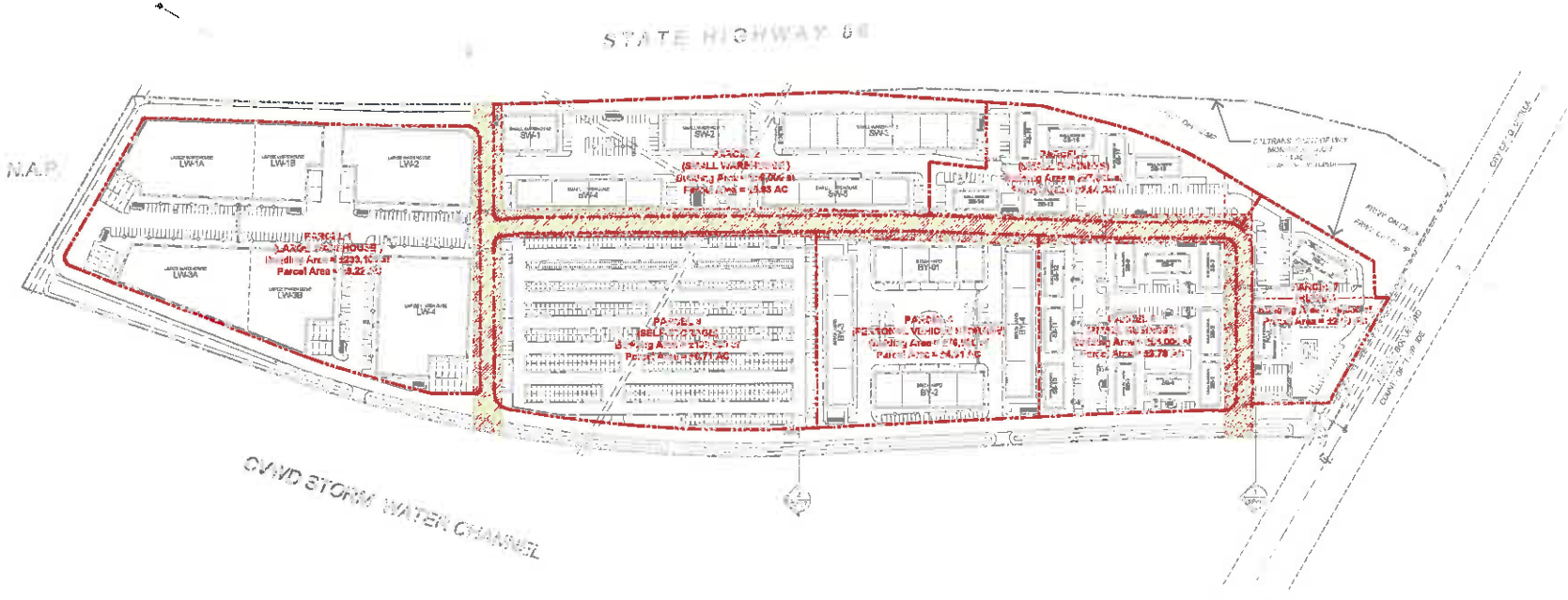
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0 752 1,505 Feet

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## Notes



**SUBDIVISION MAP**

N.W.C. OF STATE HWY. 88 &  
AIRPORT BOULEVARD  
COACHELLA, CALIFORNIA

**VICINITY MAP**

NOT TO SCALE

NORTH

**LEGEND:**

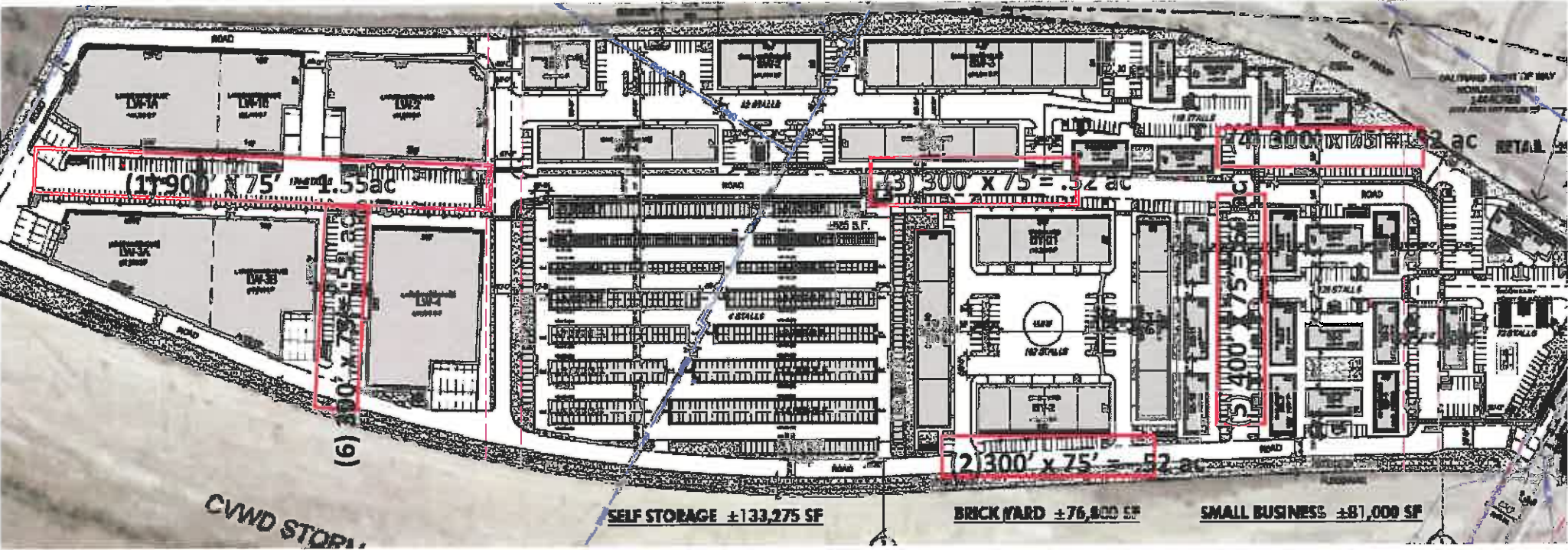
- PROPERTY LINE
- PARCEL LINE
- 60' NON-BUILD EASEMENT

**Coachella Airport Business Park**  
COACHELLA, CALIFORNIA  
**Haagen Co., LLC**  
12302 EXPOSITION BLVD., LOS ANGELES, CA 90064

**McKenty  
Malak  
ARCHITECTS**  
25 Haagen Alley, Suite 200  
Coachella, California 91103-3640  
tel: 435.505.9340 fax: 435.505.9347

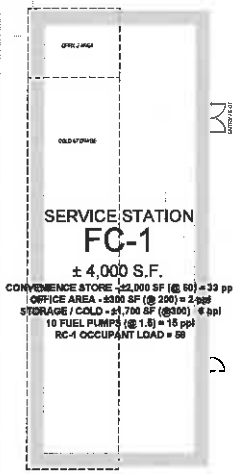
SUBDIVISION MAP  
07.14.2020 18029178A  
**SM-01**

# ALUC Open Space Exhibit Coachella Airport Business Center



**ALUC Open Space Tabulation –**  
 Open Space (1) 1.55 ac (2) .52 ac (3) .52 ac (4) .52 ac  
 (5) .69 ac (6) .52 ac = 4.32 ac

9



CONVENIENCE STORE - 22,000 SF (@ 50) = 33 ppl  
 OFFICE AREA - 8,000 SF (@ 200) = 2 ppl  
 STORAGE / COOL - 21,700 SF (@ 300) = 6 ppl  
 10 FUEL PUMPS (@ 1.8) = 18 ppl  
 RC-1 OCCUPANT LOAD = 58

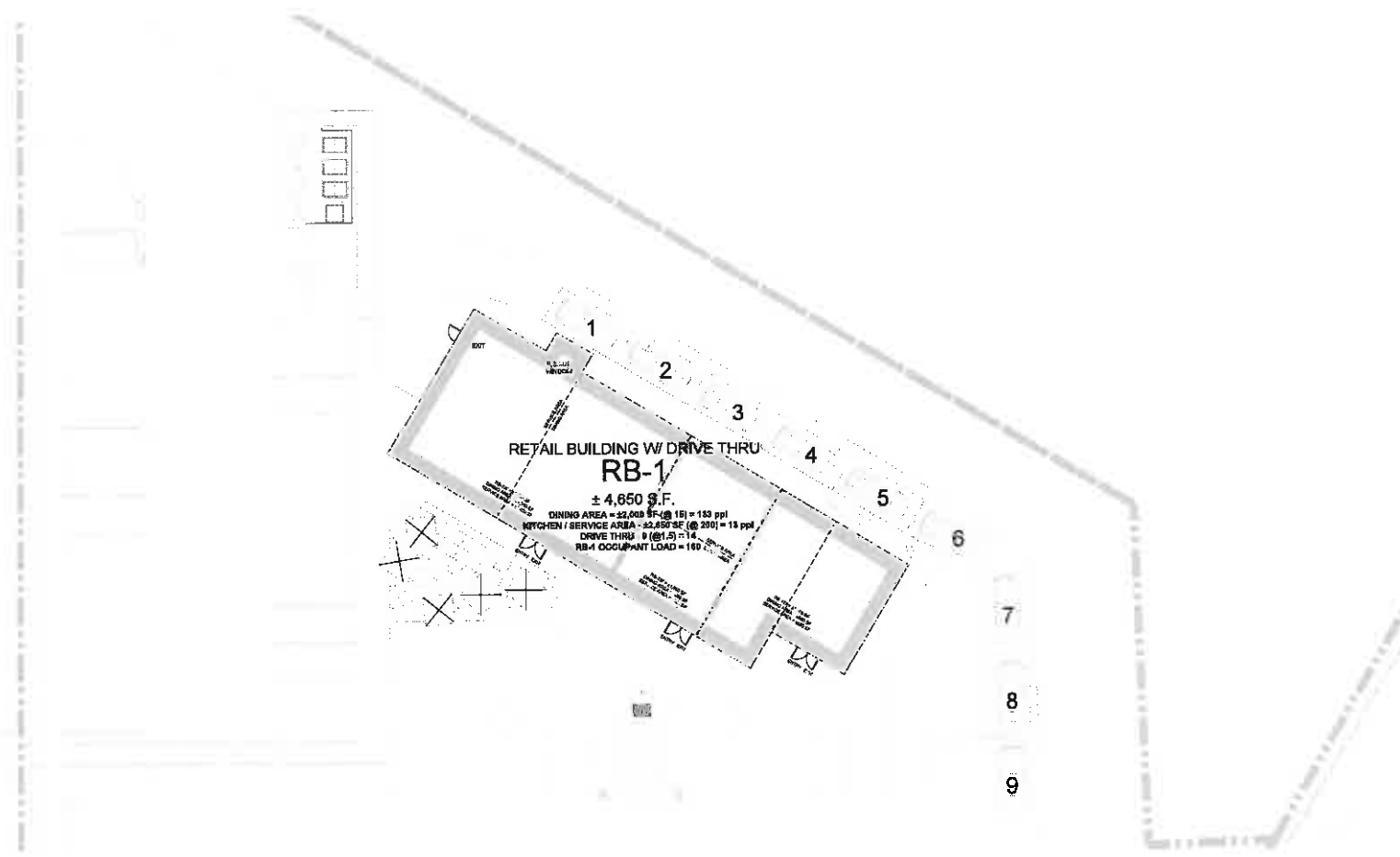


NOTE:  
 \* DEMISING WALL LOCATIONS ARE CONCEPTUAL AND SUBJECT TO CHANGE.  
 INTERIOR WALL LOCATIONS WILL VARY BASED ON TENANTS FLOOR PLAN LAYOUT

**SERVICE STATION  
SPACE ALLOCATION EXHIBIT**

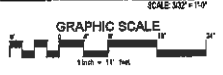






NOTE:  
 \* DEMISING WALL LOCATIONS ARE CONCEPTUAL AND SUBJECT TO CHANGE.  
 INTERIOR WALL LOCATIONS WILL VARY BASED ON TENANT'S FLOOR PLAN LAYOUT.

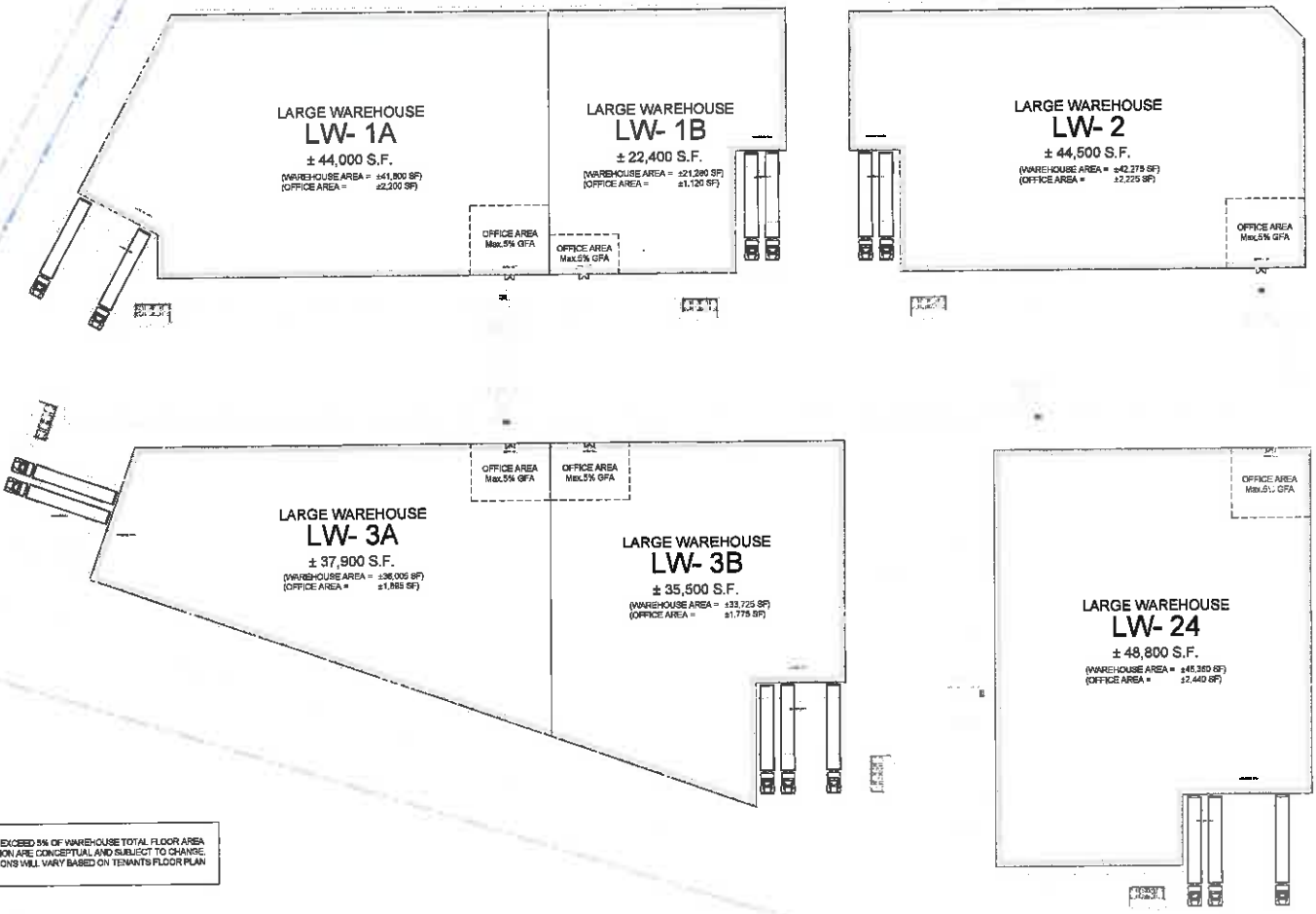
RETAIL BUILDING W/ DRIVE THRU  
**SPACE ALLOCATION EXHIBIT**



**Coachella Airport Business Park**  
 COACHELLA, CALIFORNIA  
**Haagen Co., LLC**  
 12302 EXPOSITION BLVD., LOS ANGELES, CA 90064

**McKenty  
 Malak**  
 ARCHITECTS  
 25 Haggas Alley, Santa Ana  
 Pasadena, California 91103-2648  
 TEL: 404.589.0540 FAX: 404.589.0597

EXHIBIT 'B'  
 07.21.2020 180227/MA  
**A-107b**



NOTE:  
 • OFFICE SPACE NOT TO EXCEED 5% OF WAREHOUSE TOTAL FLOOR AREA  
 • DEMISING WALL LOCATION ARE CONCEPTUAL AND SUBJECT TO CHANGE.  
 • INTERIOR WALL LOCATIONS WILL VARY BASED ON TENANTS FLOOR PLAN LAYOUT

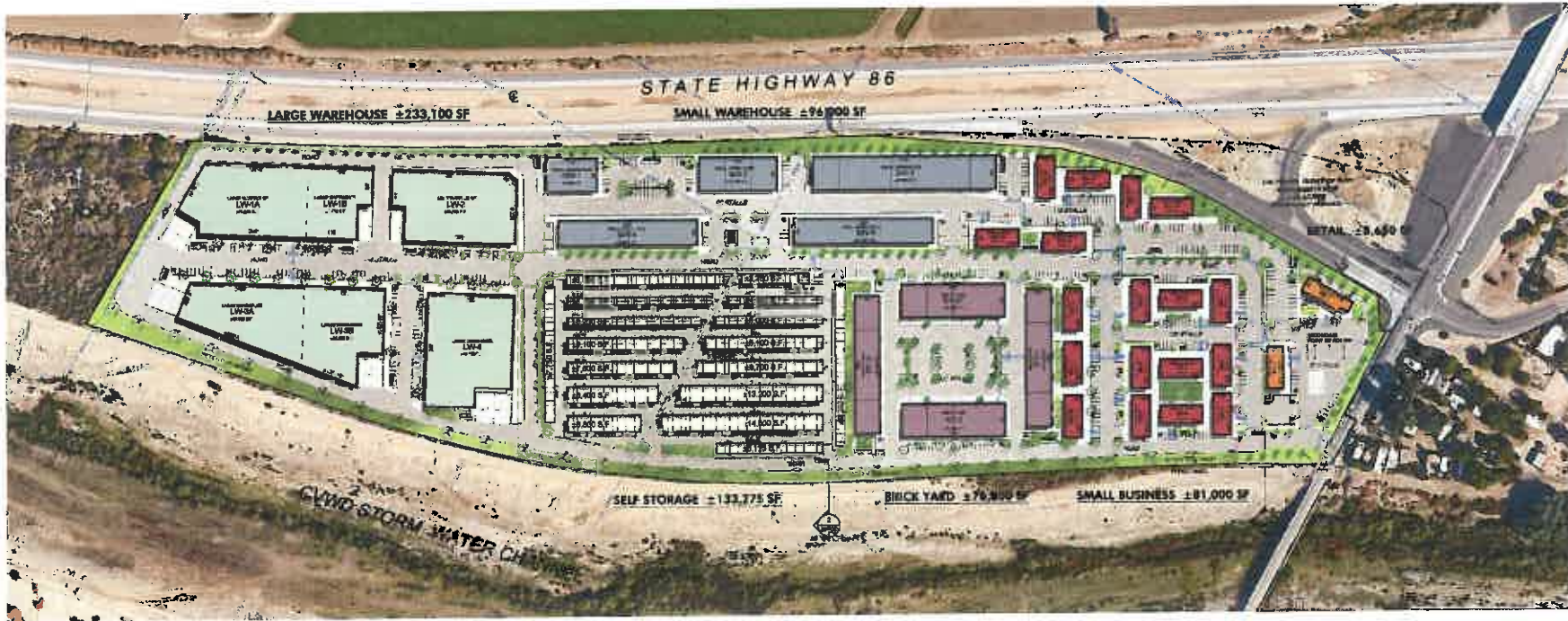
**LARGE WAREHOUSE SPACE ALLOCATION EXHIBIT**

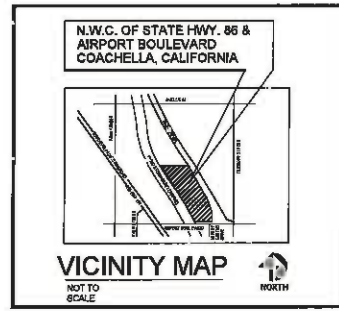
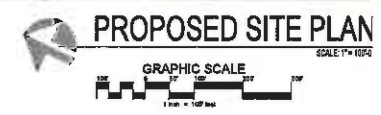
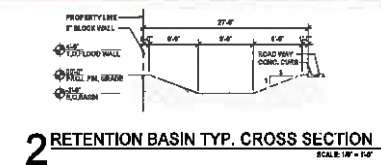
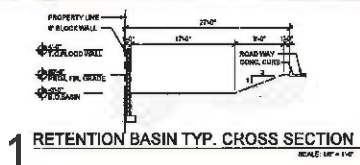
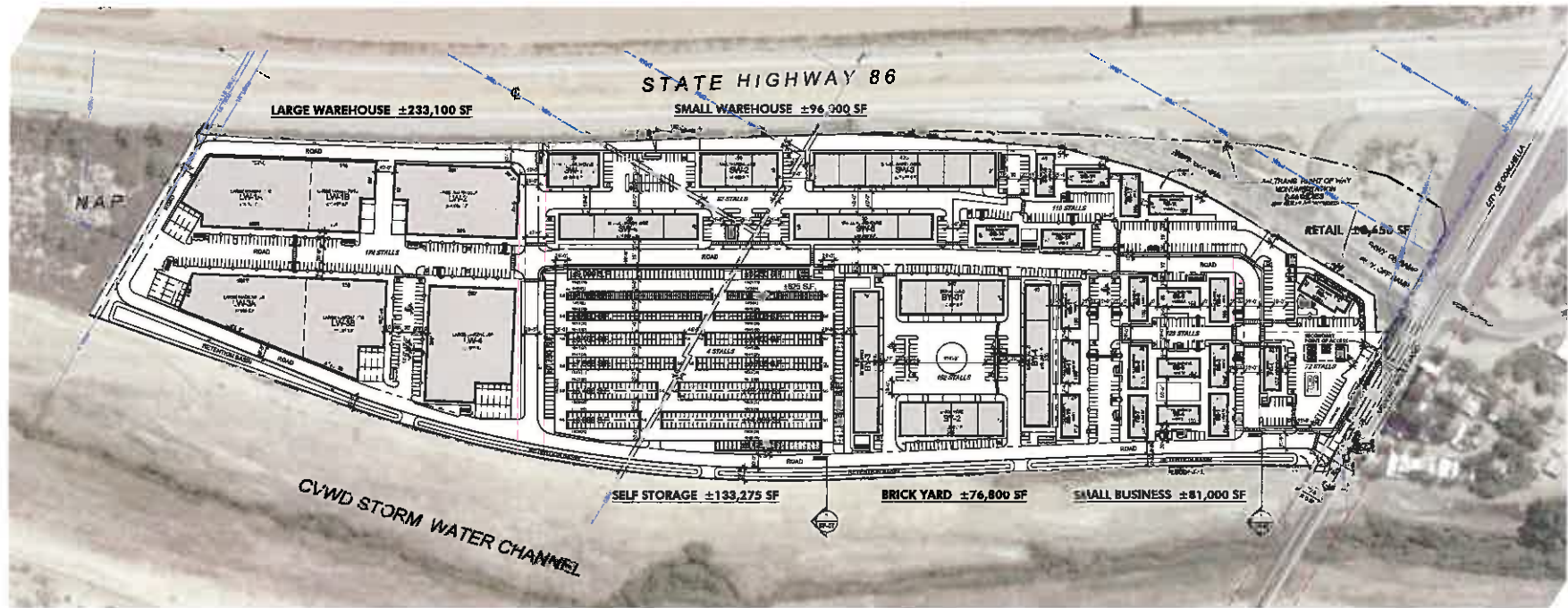


**Coachella Airport Business Park**  
 COACHELLA, CALIFORNIA  
**Haagen Co., LLC**  
 12302 EXPOSITION BLVD., LOS ANGELES, CA 90064

**McKenty**  
**Mak**  
**ARCHITECTS**  
 35 Hagen Alley, Suite 200  
 Pasadena, California 91103-2648  
 TEL: 626.992.9929 FAX: 626.992.9997

EXHIBIT 'A'  
 07.30.2020 1902BTMA  
**A-107a**



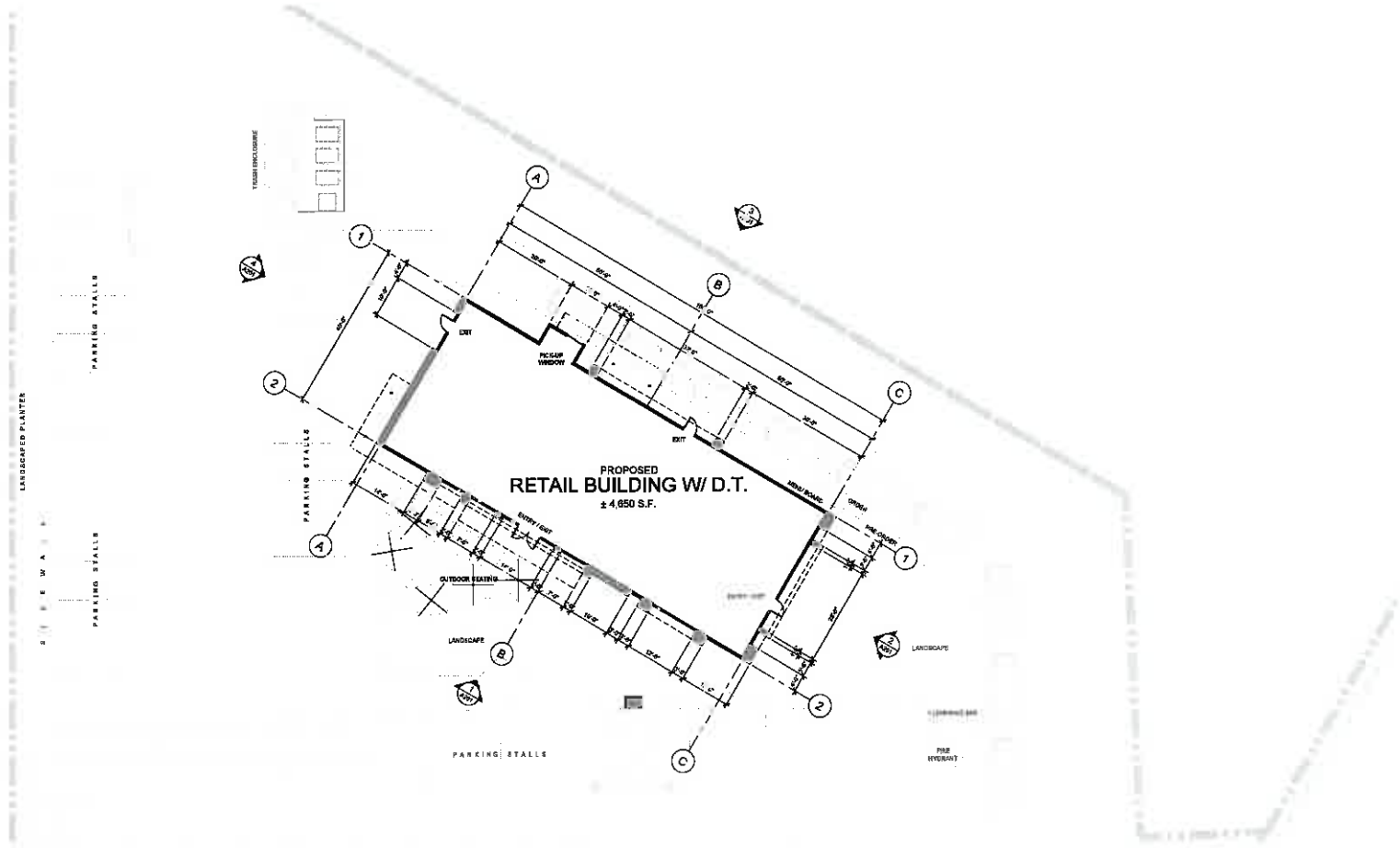


PROJECT DATA	
CLIENT:	HAAGEN CO., LLC
PROJECT LOCATION:	N.W.C. OF STATE HWY. 86 AND AIRPORT BOULEVARD COACHELLA, CA 92274
JURISDICTION:	CITY OF COACHELLA, CA
APN#:	763-330-013, 018, 020
ZONING:	EXISTING: MH (HEAVY INDUSTRIAL) AND MANUFACTURING SERVICE (M) INDUSTRIAL PARK OVERLAY DIST. PROPOSED: I-P (INDUSTRIAL PARK OVERLAY DIST.)
PROPOSED USE:	LARGE & SMALL WAREHOUSE, SMALL BUSINESS, SELF STORAGE, SERVICE STATION AND DRIVE THROUGH
BOUNDARY INFORMATION:	THIS PLAN HAS BEEN PREPARED BY USING THE CONSTRAINTS EXHIBIT BY THE ALUM GROUP, DATED FEB. 16, 2018

SITE SUMMARY	
<b>SITE AREA</b>	21.56 AC
APN# 763-330-013	8.67 AC
APN# 763-330-018	11.18 AC
APN# 763-330-020	11.18 AC
<b>TOTAL SITE AREA</b>	<b>42.98 AC</b>
<b>BUILDING DATA</b>	
<b>PROPOSED BUILDING AREA</b>	
LARGE WAREHOUSE	±233,100 SF
SMALL WAREHOUSE	±96,000 SF
SMALL BUSINESS	±81,000 SF
BRICK YARD	±76,800 SF
SELF STORAGE	±133,275 SF
SERVICE STATION/ MINI MART	±4,000 SF
DRIVE-THRU FAST FOOD RESTAURANT	±4,650 SF
<b>TOTAL BUILDING AREA</b>	<b>±628,825 SF</b>
RETENTION BASIN	±6F

BUILDING DATA	
<b>PROPOSED BUILDING HEIGHT</b>	
LARGE WAREHOUSE	±30' TO 50' HIGH
SMALL WAREHOUSE	±28' TO 32' HIGH
SMALL BUSINESS	±24' TO 28' HIGH
BRICK YARD	±24' TO 28' HIGH
SELF STORAGE	±24' TO 28' HIGH
RETAIL (GAS STATION & DRIVE THRU)	±24' TO 28' HIGH
<b>BUILDING TYPE</b>	<b>TYPE V-B (FULLY SPRINKLERED)</b>
<b>NOTE:</b>	
1. FIRE HYDRANT TO BE LOCATED MAX. 300 FT. O.C. OR AS APPROVED BY RIVERSIDE COUNTY FIRE DEPARTMENT	
2. PROPOSED FIRE HYDRANT LOCATION ON SITE AND REPRESENTED BY FH SYMBOL	
3. 43 FT. LENGTH TEMPLATE WAS USED TO ILLUSTRATE FIRE TRUCK PATH	
4. MINIMUM TRUCK TURNING RADIUS 200 FT.	

PARKING REQUIRED	
Required for Drive Through (154% of Customer Area + 1200% of Non Customer Area)	
Customer Area (±2,000 SF - 50%)	±44.4 Stalls
Non Customer Area (±2,000 SF - 50%)	±10 Stalls
Service Station	±5 Stalls
Large Warehouse (±27,200 sf)	20,000 sf = 50 Stalls
	213,100 sf = 213.1 Stalls
Small Warehouse (96,000 sf)	20,000 sf = 50 Stalls
	76,000 sf = 76 Stalls
Small Business (81,000 sf)	20,000 sf = 50 Stalls
	61,000 sf = 61 Stalls
Brick Yard Car Storage (76,800 sf)	20,000 sf = 50 Stalls
	56,800 sf = 56.8 Stalls
Self Storage Office (±25 sf)	±1.5 Stalls
<b>GRAND TOTAL</b>	<b>857.9 Stalls or 858 Stalls</b>
<b>TOTAL PARKING PROVIDED</b>	<b>857 Stalls</b>



RETAIL BUILDING W/ DRIVE THRU  
**FLOOR PLAN**  
 SCALE: 3/32" = 1'-0"  
 GRAPHIC SCALE  
 1" = 10' 0"

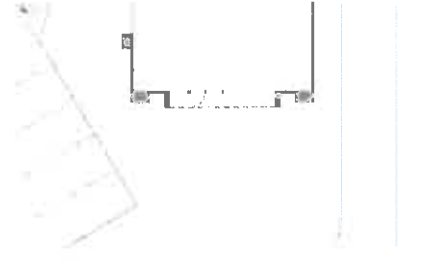
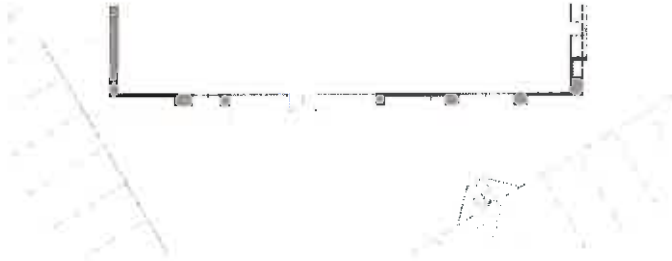


**Haagen Co LLC**  
 12302 Exposition Boulevard, Los Angeles CA 90064

**Coachella Airport Business Park**  
 NWC State Highway 86 and Airport Road  
 COACHELLA, CALIFORNIA

**McKently  
 Malak**  
 ARCHITECTS  
 37700 Magnolia Valley Center Road  
 Palmdale, California 91367  
 760.250.9300 FAX 760.250.9307

**FLOOR PLAN**  
 06.09.2020 15028TMA  
**A-101**

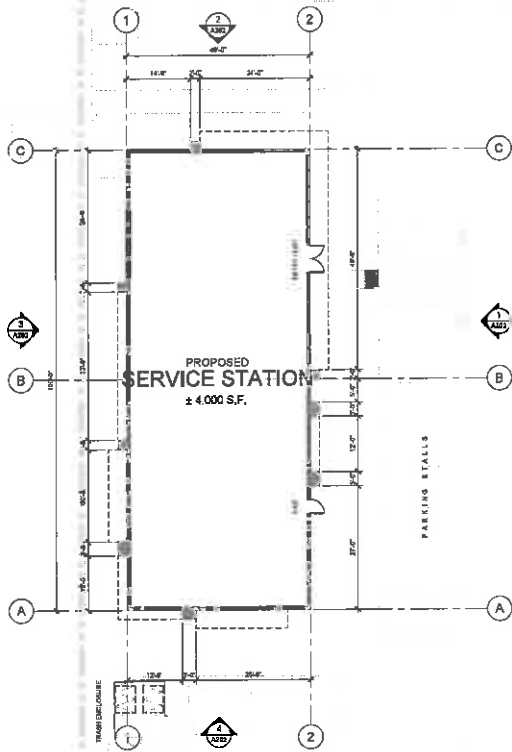
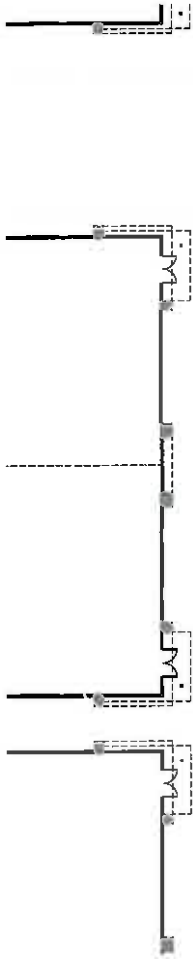


**MATERIALS AND FINISH KEYNOTES**

- ① EXTERIOR CORNER "LATEST" MESH PANEL
- ② EXTERIOR CORNER "LATEST" MESH PANEL
- ③ FLOOR PANEL - IDENTIFY TO
- ④ WALL PANEL - IDENTIFY TO
- ⑤ ALUMINUM CLADDING SYSTEM - IDENTIFY TO
- ⑥ ALUMINUM CLADDING SYSTEM - IDENTIFY TO
- ⑦ ALUMINUM CLADDING SYSTEM - IDENTIFY TO
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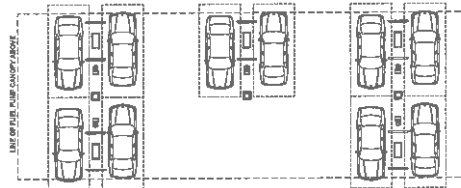


Note: Signage shown on the elevations for graphic purpose only and does not represent the use or actual tenants



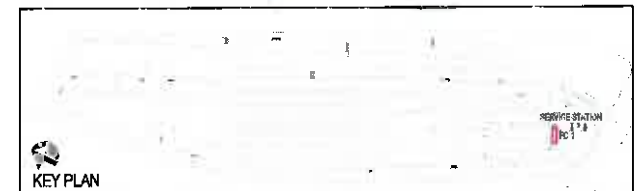
PARKING STATUS

PRE HYDRANT



LINE OF CONCRETE SUB

UNDERGROUND RAIL TRANSIT



KEY PLAN

**SERVICE STATION  
FLOOR PLAN**

SCALE: 3/32" = 1'-4"

GRAPHIC SCALE



**Haagen Co LLC**  
12302 Exposition Boulevard, Los Angeles CA 90064

**Coachella Airport Business Park**  
NWC State Highway 86 and Airport Road  
COACHELLA, CALIFORNIA

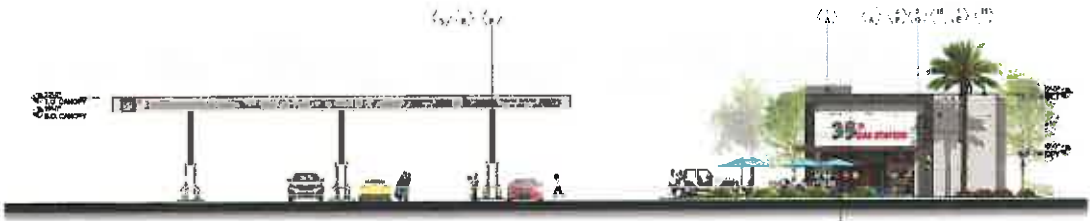
**McKenty Malak**  
ARCHITECTS  
35 HUNTER AVENUE SUITE 200  
PASADENA, CALIFORNIA 91103  
714.343.1340 6-0216930307

**FLOOR PLAN**  
06.08.2020 1802RTMA

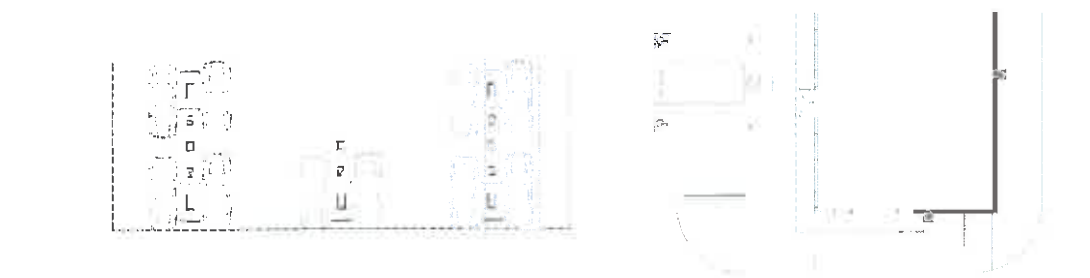
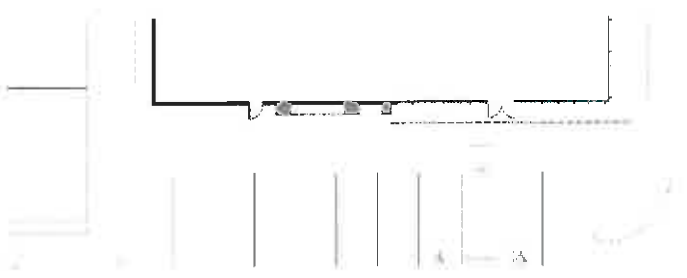
**A-102**



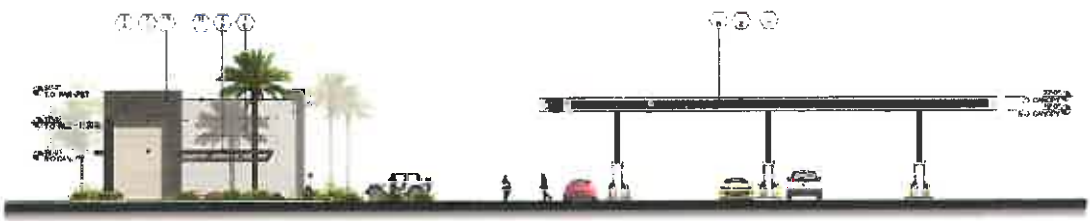
1 Front Elevation - Service Station



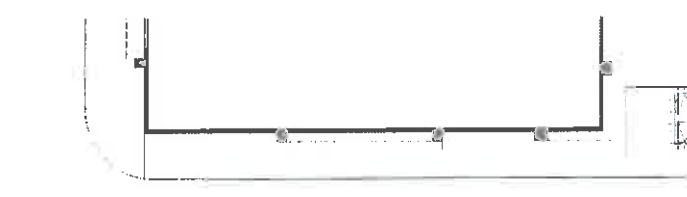
2 Right Side Elevation



3 Rear Elevation

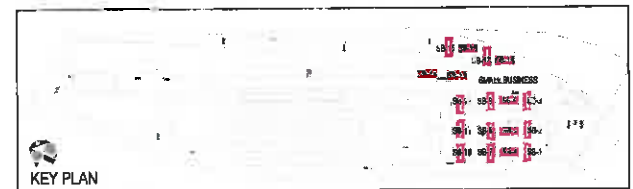
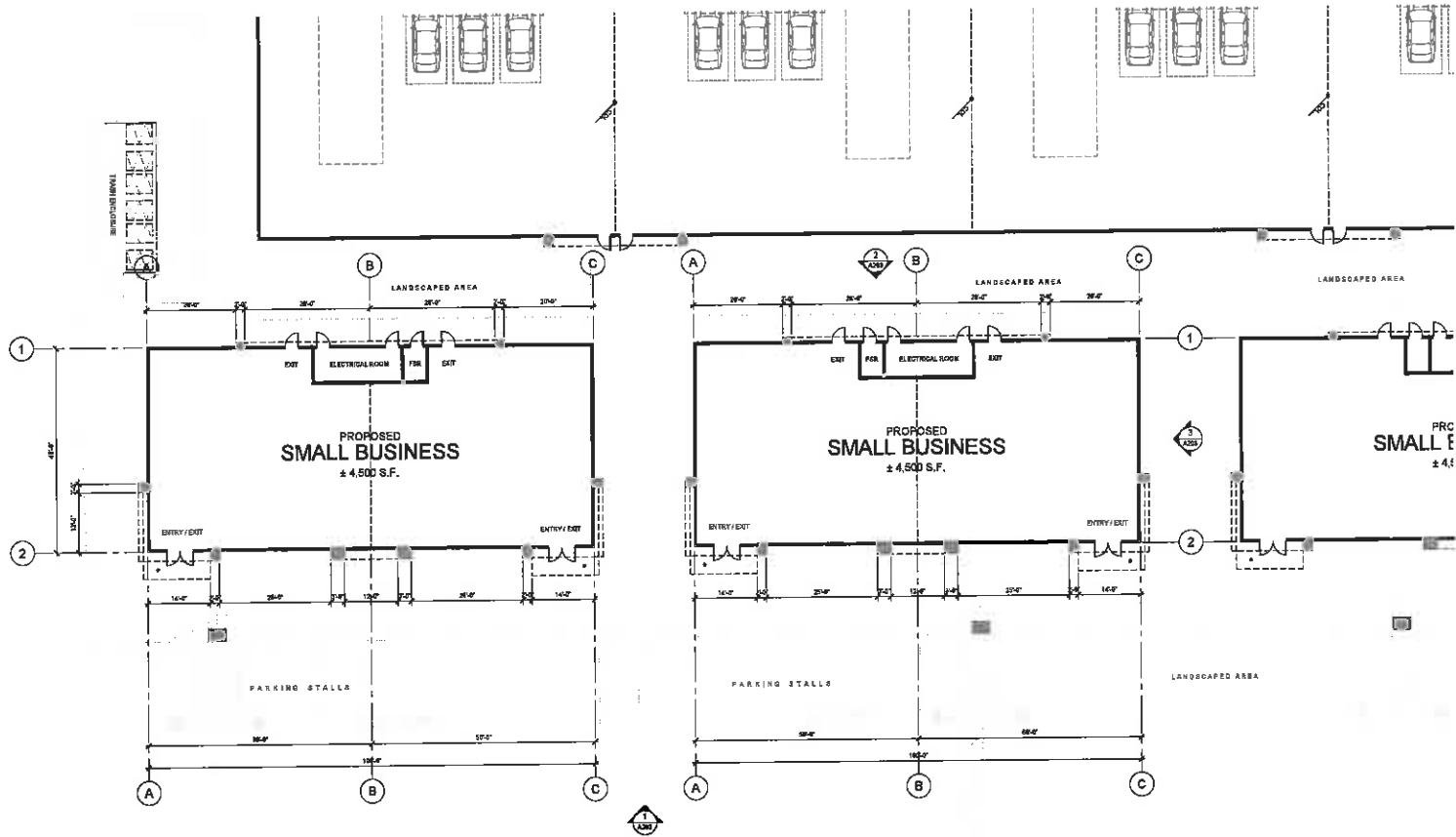


4 Left Side Elevation



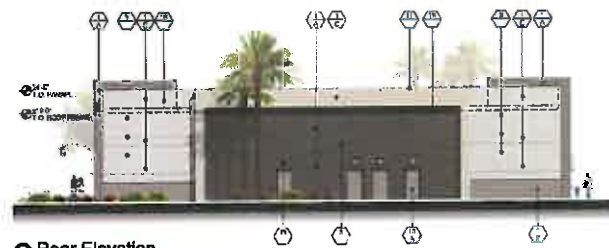
Note: Signage shown on the elevations for graphic purpose only and does not represent the use or actual tenants



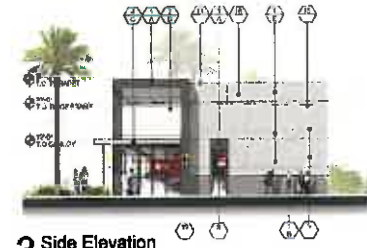
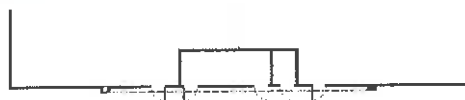




**1 Front Elevation - Typ. Small Business**  
SCALE 1/8" = 1'-0"



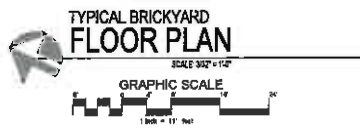
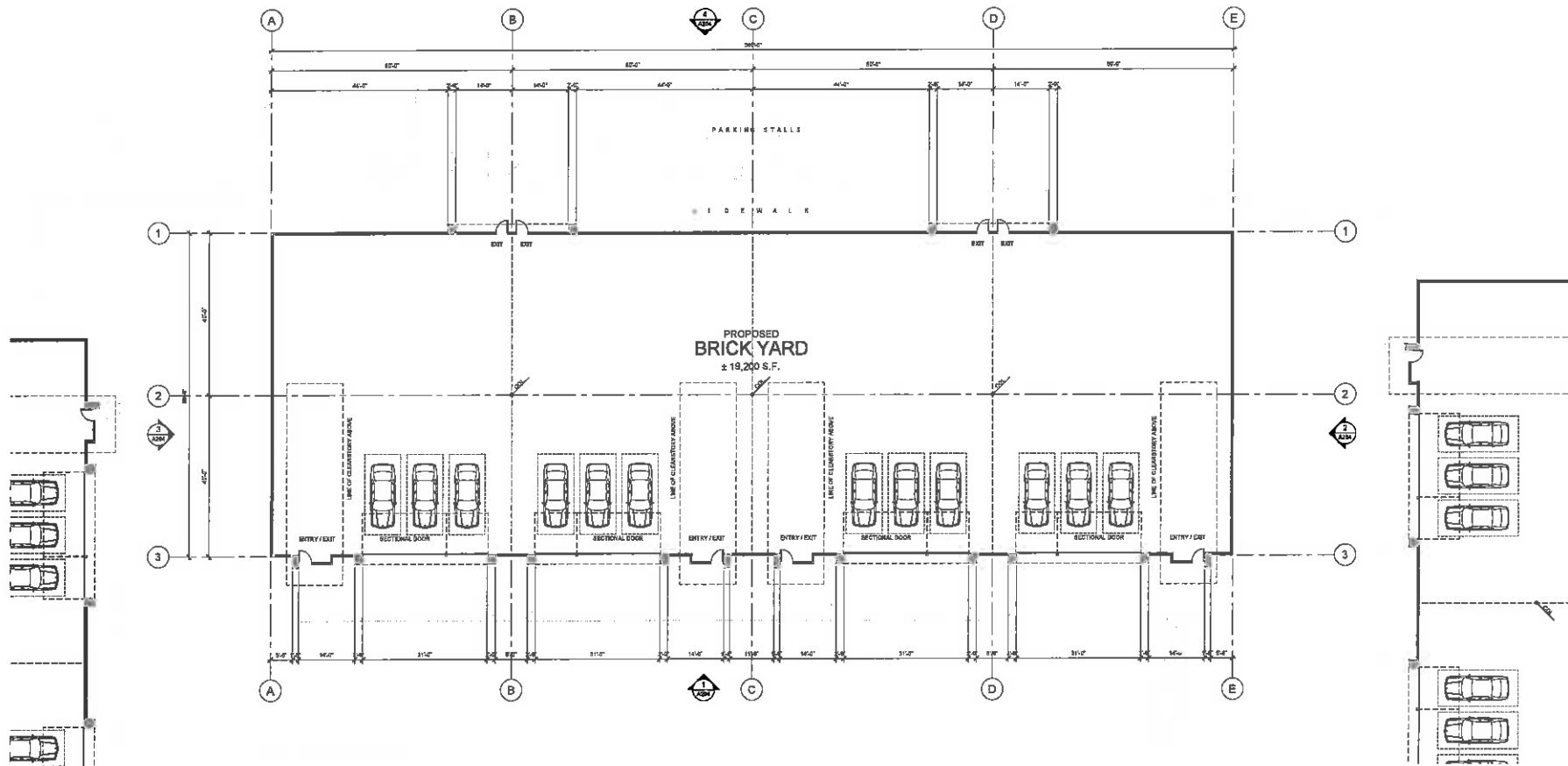
**2 Rear Elevation**  
SCALE 1/8" = 1'-0"



**3 Side Elevation**  
SCALE 1/8" = 1'-0"



Note: Signage shown on the elevations for graphic purpose only and does not represent the use or actual tenants

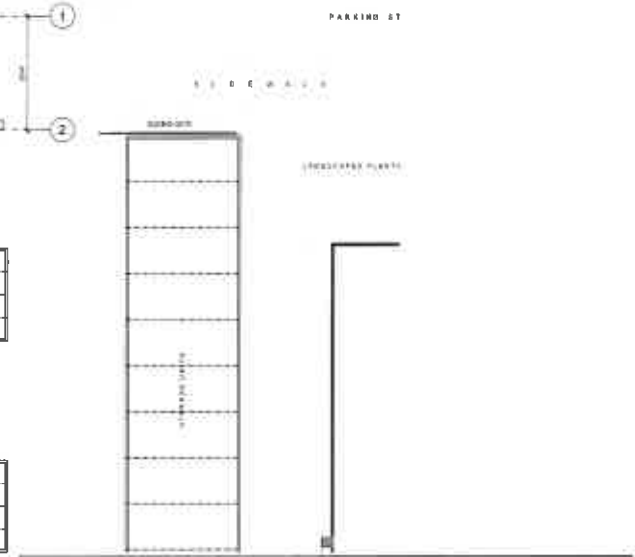
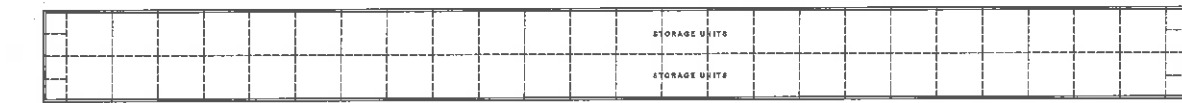
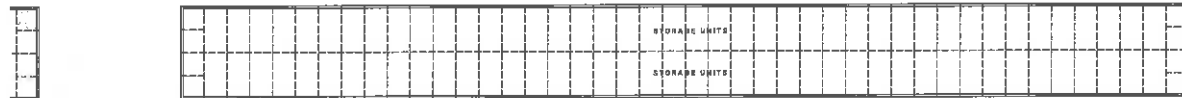




LANDSCAPE PLANTER

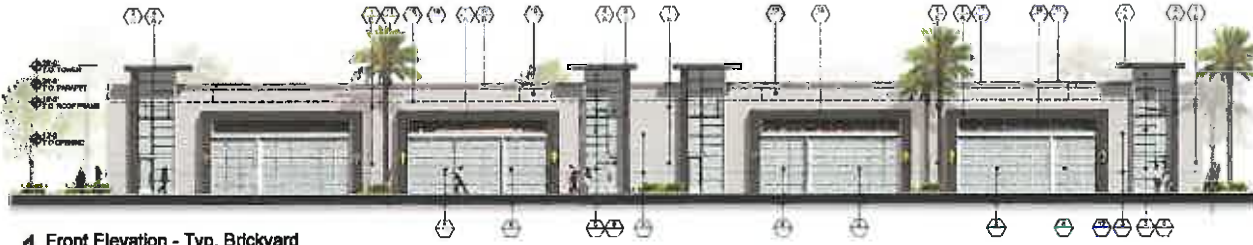


PARKING ST



TYPICAL SELF STORAGE FLOOR PLAN  
SCALE 3/32" = 1'-0"  
GRAPHIC SCALE  
1 inch = 11' 0"

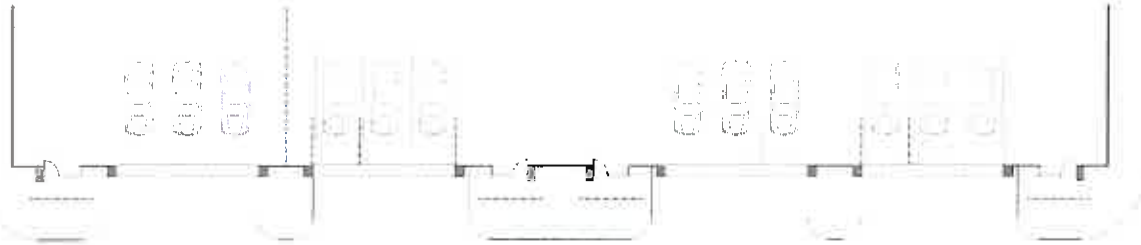




**1 Front Elevation - Typ. Brickyard**  
SCALE 1/8" = 1'-0"



**2 Right Side Elevation**  
SCALE 1/8" = 1'-0"



**3 Left Side Elevation**  
SCALE 1/8" = 1'-0"

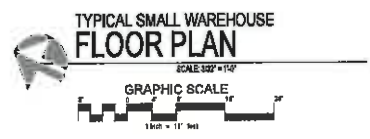
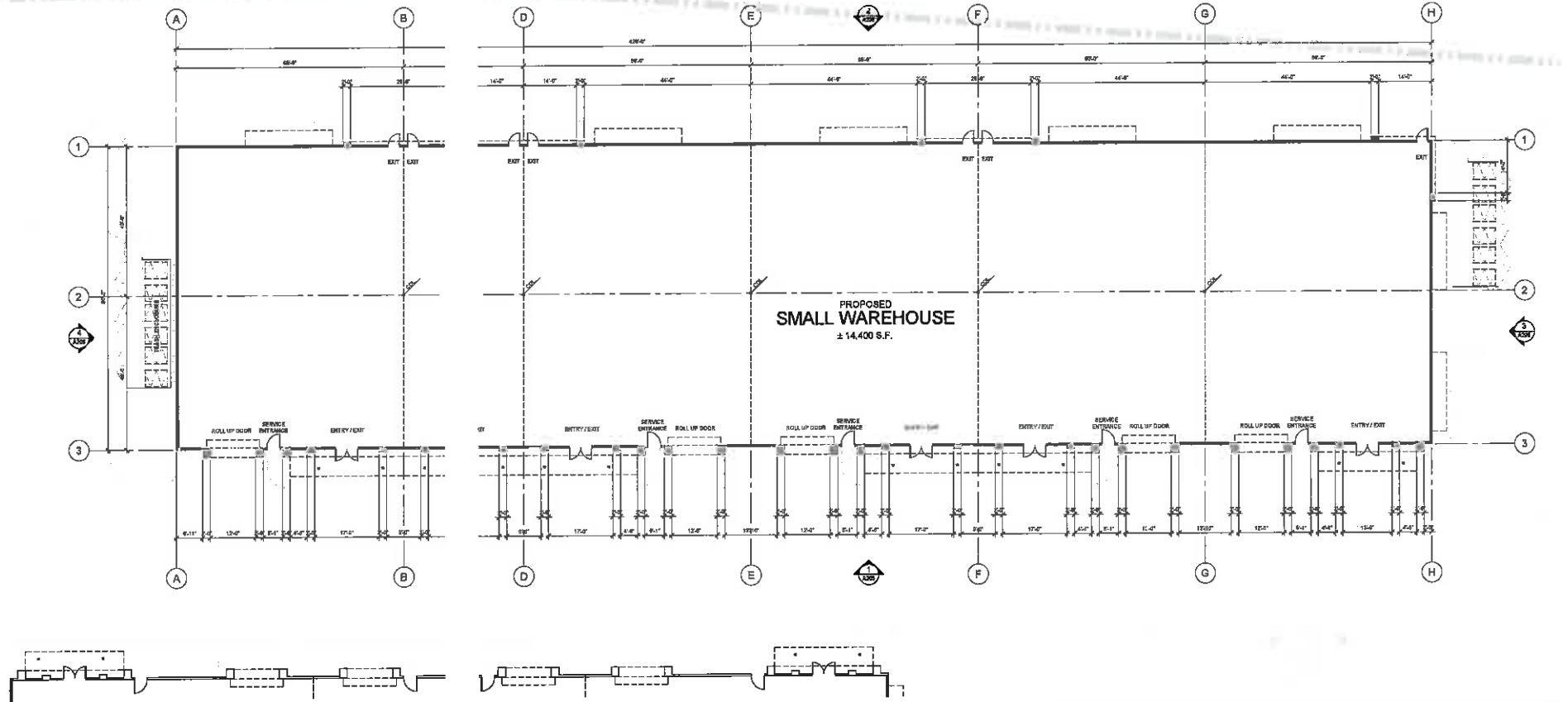


**4 Rear Elevation**  
SCALE 1/8" = 1'-0"



**5 Side Elevation - Typ. Self Storage**  
SCALE 1/8" = 1'-0"

Note: Signage shown on the elevations for graphic purpose only and does not represent the use or actual tenants

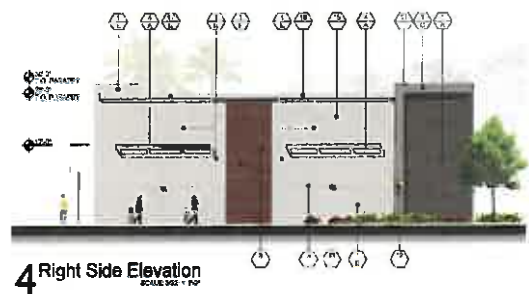
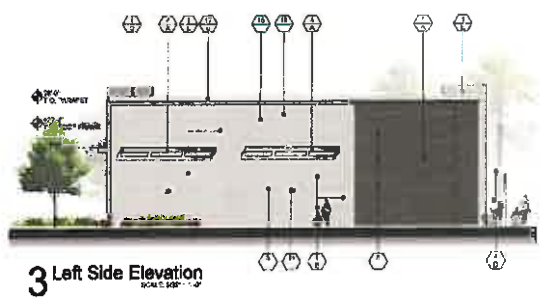
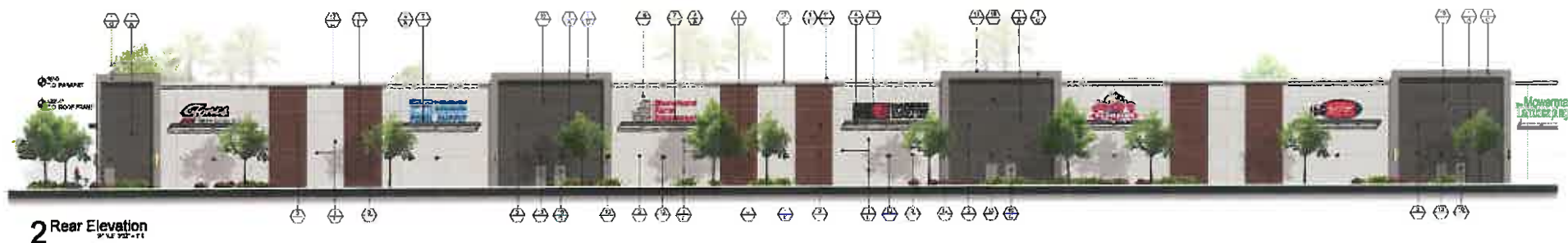
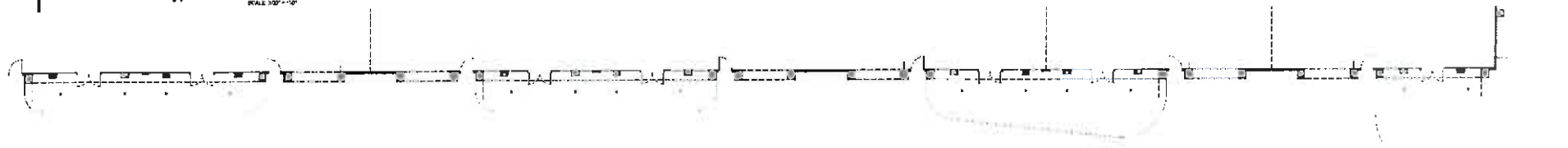


**Haagen Co LLC**  
 12302 Exposition Boulevard, Los Angeles CA 90064

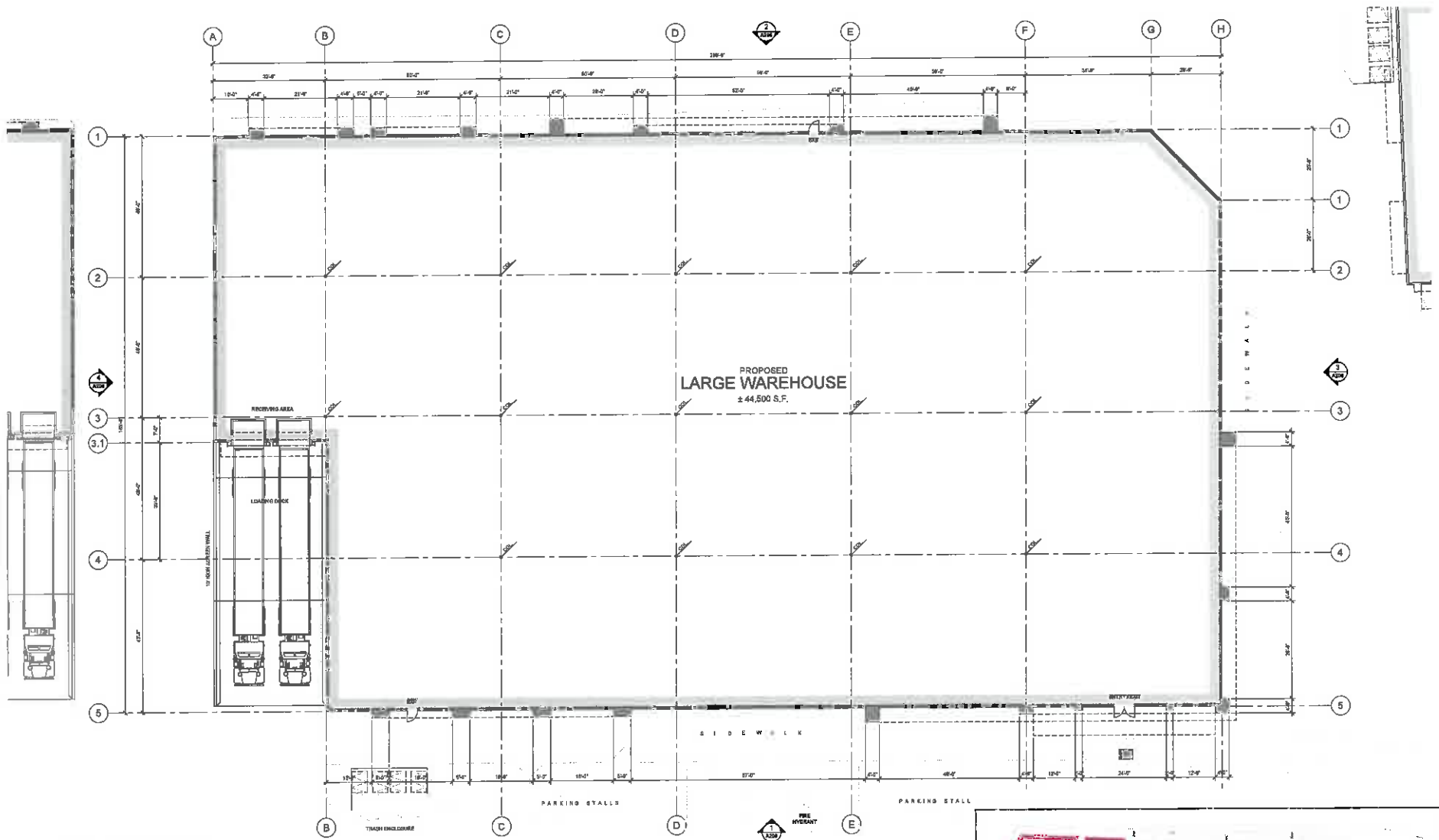
**Coachella Airport Business Park**  
 NWC State Highway 86 and Airport Road  
 COACHELLA, CALIFORNIA

**McKenty Malak**  
 ARCHITECTS  
 33 HUGO AVE SUITE 200  
 PALMDALE, CALIFORNIA 91303  
 1-805-818-1410 P-765333297

**FLOOR PLAN**  
 06.08.2020 18028TMA  
**A-105**



Note: Signage shown on the elevations for graphic purpose only and does not represent the use or actual tenants



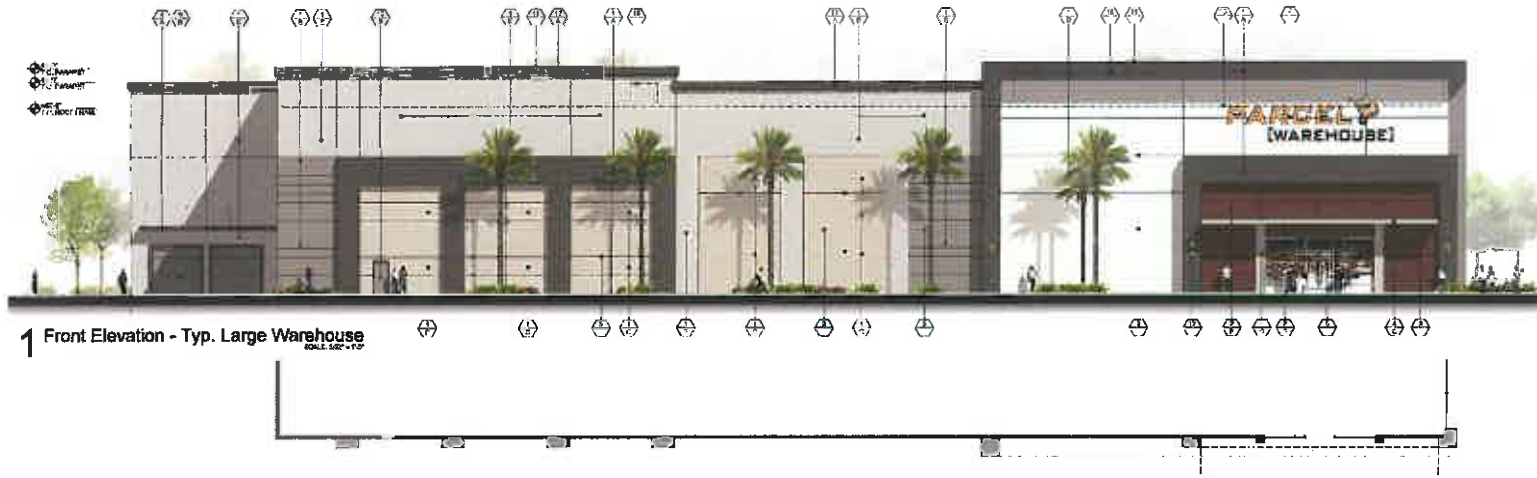
**Haagen Co LLC**  
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**Coachella Airport Business Park**  
 NWC State Highway 86 and Airport Road  
 COACHELLA, CALIFORNIA

**McKently Malak**  
 ARCHITECTS  
 35 Huguenot Alley Suite 200  
 Pasadena, California 91103  
 T: 626.797.4848 F: 626.797.4847

**FLOOR PLAN**  
 06.09.2020 180281MA  
**A-106**

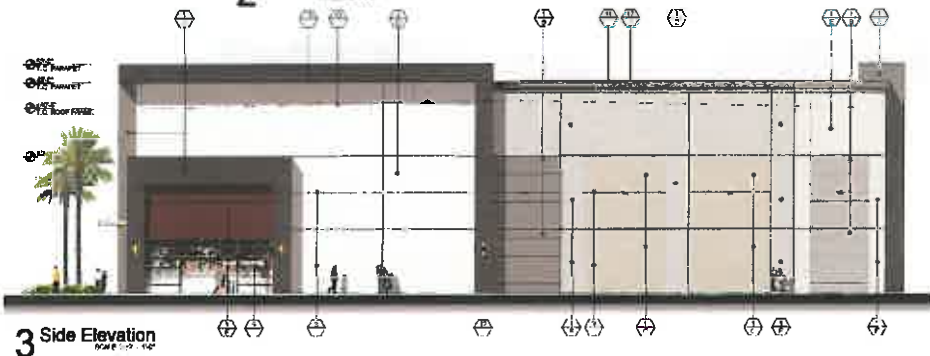




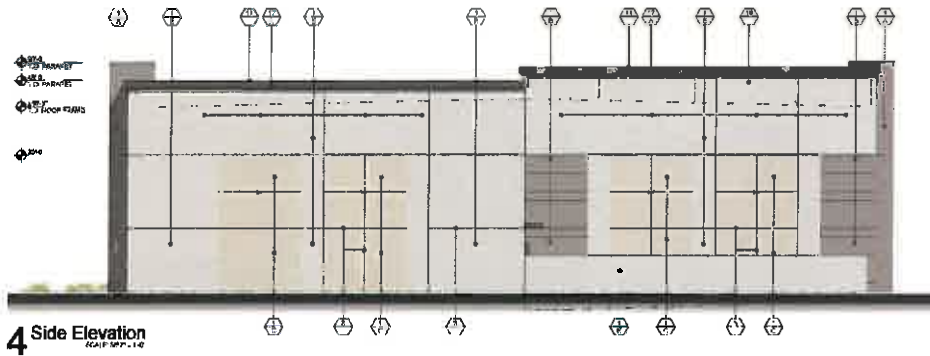
1 Front Elevation - Typ. Large Warehouse  
SCALE: 1/8" = 1'-0"



2 Rear Elevation  
SCALE: 1/8" = 1'-0"

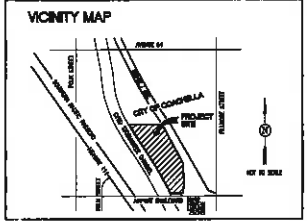
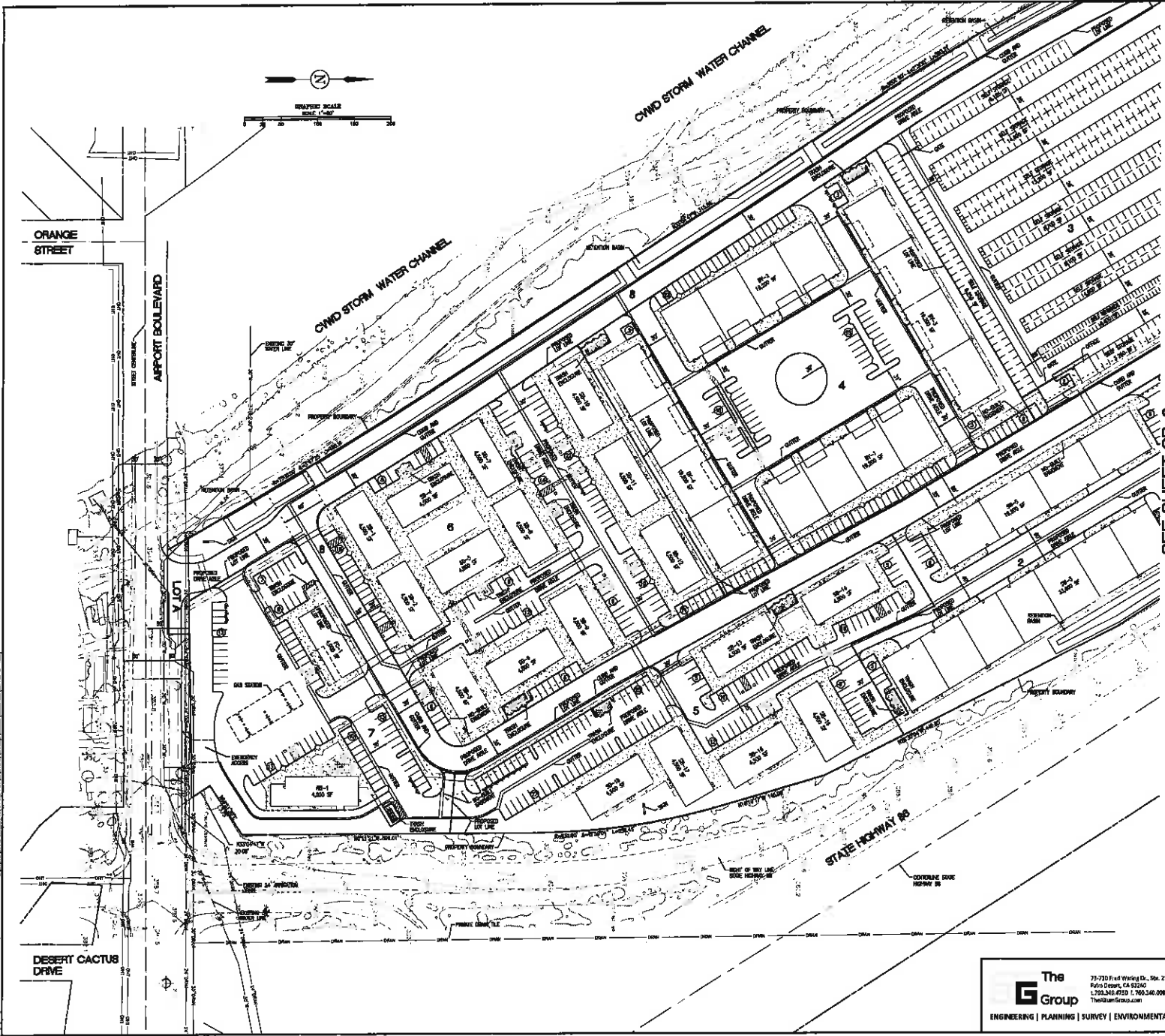


3 Side Elevation  
SCALE: 1/8" = 1'-0"



4 Side Elevation  
SCALE: 1/8" = 1'-0"

Note: Signage shown on the elevations for graphic purpose only and does not represent the use or actual tenants



**LEGEND**

[Symbol]	LINE OF WAY LINES
[Symbol]	CENTERLINE
[Symbol]	EXISTING CONTOUR
[Symbol]	EXISTING OVERHEAD TELEPHONE
[Symbol]	EXISTING UNDERGROUND ALLEYWAY
[Symbol]	EXISTING PRIVATE DRAIN TILE LINE
[Symbol]	EXISTING WATER LINE
[Symbol]	EXISTING SEWER LINE
[Symbol]	15' MIN.
[Symbol]	90'-1'
[Symbol]	FEET COVER
[Symbol]	SMALL BUSINESS
[Symbol]	LARGE WAREHOUSE
[Symbol]	SMALL WAREHOUSE
[Symbol]	BRICK VEH
[Symbol]	PROPOSED CONCRETE SIDEWALK
[Symbol]	PROPOSED AC PAVEMENT
[Symbol]	LOT NUMBER
[Symbol]	PARKING SPACES

**PARKING TABLE**

STANDARD PARKING SPACES	501 SPACES
AIR PARKING SPACES	44 SPACES
CLEAR AREA/PARKING BY ASSESSMENT	43 SPACES
BY WAY OF STANDARD BY ASSESSMENT	8 SPACES
TOTAL PARKING PROVIDED	596 SPACES
TOTAL PARKING REQUIRED	596 SPACES

**APN:** 785-000-011, 785-000-018 & 785-000-020

**APPLICANT:**  
 HAAGEN COMPANY, LLC  
 CONTACT: CHRIS FINNEY, PRESIDENT AND CHIEF OPERATING OFFICER  
 12001 BENTLEY BLVD., SUITE 200  
 LOS ANGELES, CA 90044  
 TEL: (310) 550-1200  
 EMAIL: CHRIS@HAAGEN.COM

**EXISTING ZONE:** EXISTING ZONING M-H (MAY BE FUTURE)

**PROPOSED ZONE:** M-2 (MAY BE A-F OVERLAY), L-P (BUSINESS PARK OVERLAY), M-2 (MANUFACTURING OVERLAY)

**FEMA FLOOD ZONE:** FEMA FLOOD ZONE - FLOOD ZONE AS SHOWN ON SPECIAL FLOOD HAZARD AREAS WITH FLOOD ELEVATION (SFE)

- EXHIBIT NOTES**
- TOPOGRAPHIC SHOWN SECTION IS PROVIDED BY JEROMEAN SURVEY, INC. ON JANUARY 11, 2018.
  - AIRPORT CORP/PROPERTY OWNERS ASSOCIATION ZONING ZONE M.
  - RECORDS FOR CITY OF COACHELLA PROPERTY DEVELOPMENT ORDINANCES (CODES M-10):
    - WHERE AN M-2 ZONING ADULTS UPON A STREET OR HIGHWAY WHERE TWO-BARRIERS OF THE PROPERTY IN THIS BLOCK ON THE OPPOSITE SIDE OF THE STREET IS ZONED RESIDENTIAL, A YIELD SHALL BE PROVIDED OF 250 (20) FEET IN WIDTH.
    - WHERE AN M-2 ZONING ADULTS UPON A STREET OR HIGHWAY AND THERE ONE OR BOTH ADJACING ZONES ARE RESIDENTIAL, A YIELD SHALL BE PROVIDED THREE (3) FEET IN WIDTH IN ADDITION TO THE YIELD OF THE ADJACENT ZONES.
    - WHERE AN M-2 ZONING ADULTS UPON A PRIVATE DRIVEWAY AS DESCRIBED ON THE DECLARATION ELEMENT OF THE RECORDS PLAN, A YIELD SHALL BE PROVIDED OF 250 (20) FEET IN WIDTH.
    - WHERE AN M-2 ZONING ADULTS A RESIDENTIAL ZONE, THERE SHALL BE A YIELD OF NOT LESS THAN TWENTY (20) FEET ADJACENT THIS ZONE. THIS SHALL BE REDUCED TO TWENTY (20) FEET WHERE THERE EXISTS A PUBLIC ALLEY ADJACENT THE TWO ZONES.
  - RECORDS FOR CITY OF COACHELLA PROPERTY DEVELOPMENT ORDINANCES (CODES L-1-P):
    - 30 FOOT BUFFER FOR IMPROVED DRIVE.
    - 18 FOOT LANDSCAPE PLANTER ADJACENT TO THE DRIVE DRIVEWAY.
    - LANDSCAPE PLANTER NEXT TO LANDSCAPE SPACE SHOULD BE 5 FEET WIDE WITH BUFFER.
    - 5 FOOT PLANTER ADJACENT TO DRIVE DRIVE DRIVE AREA.

**UTILITIES**

WATER	COACHELLA VALLEY WATER DISTRICT
SEWER	COACHELLA VALLEY WATER DISTRICT
WATER, SEWERAGE DISTRICT	PHONE (760) 388-0222
PHONE (760) 388-0222	
DRIVE	WESTERN CALIFORNIA GAS COMPANY
PHONE (909) 335-7907	
PHONE (909) 335-7907	
BIWATER	COACHELLA VALLEY WATER DISTRICT
PHONE (760) 388-0222	
PHONE (760) 388-0222	
PHONE (760) 388-0222	
PHONE (760) 388-0222	

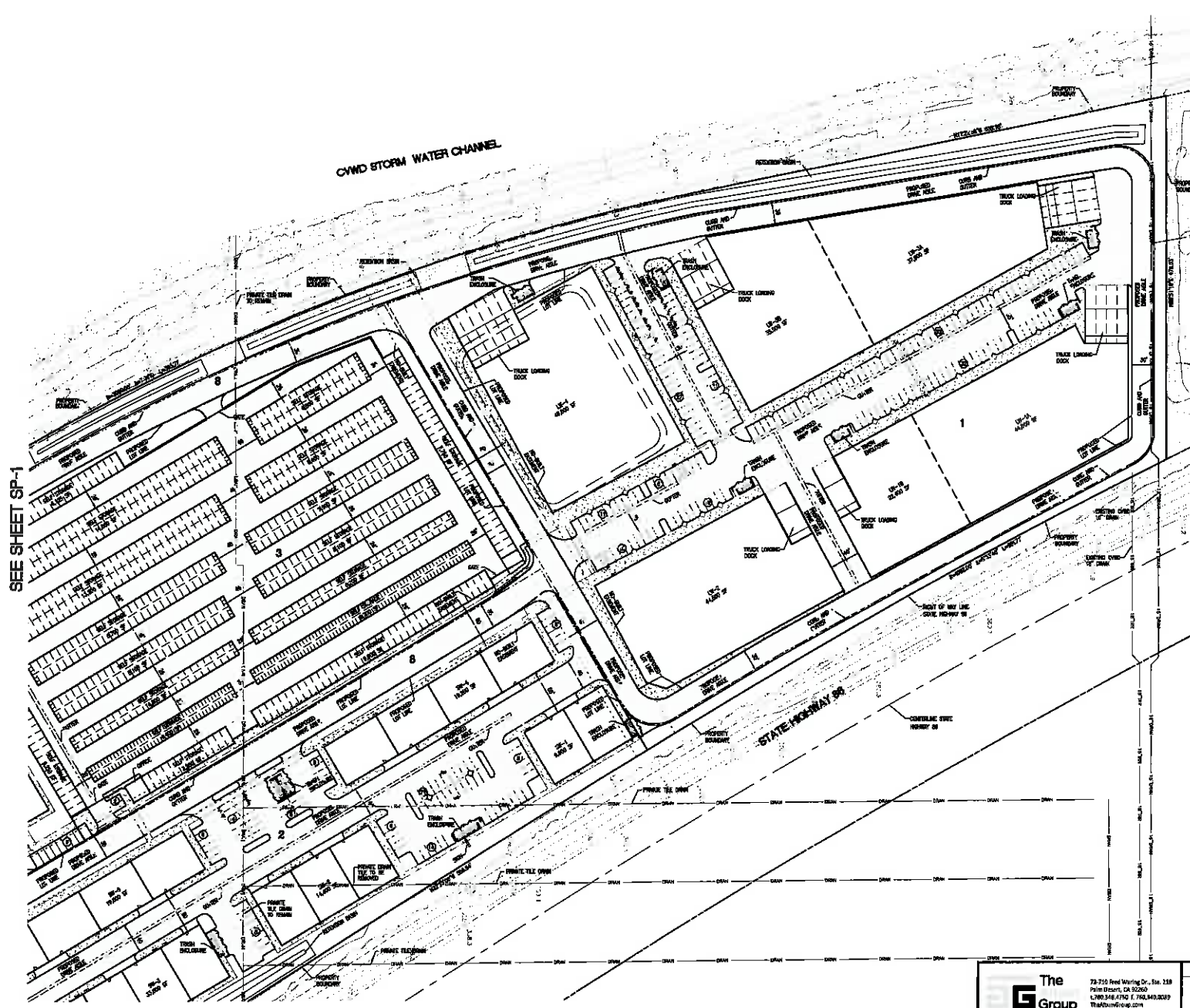
**LEGAL DESCRIPTION**  
 SOUTH PORTION OF THE SOUTHWEST QUARTER AND THE SOUTHWEST QUARTER OF SECTION 18, TOWNSHIP 8 NORTH, RANGE 8 EAST, 39N SEWERAGE DRAINAGE AND SEWERS IN THE COUNTY OF KERN, STATE OF CALIFORNIA, ACCORDING TO THE OFFICIAL PLAN THEREOF.

<p>73-730 Ford Writing Dr., Ste. 215        Palms Springs, CA 92260        1.760.345.6722   1.760.345.0068        TheGroup.com</p> <p>ENGINEERING   PLANNING   SURVEY   ENVIRONMENTAL</p>	<p><b>CITY OF COACHELLA, CA</b></p> <p><b>PRELIMINARY SITE PLAN</b></p> <p><b>COACHELLA AIRPORT BUSINESS PARK</b></p> <p>BEING A PORTION OF THE SOUTH HALF OF SECTION 18, T8S, R8E, S8W.</p>	<p>SHEET NO.</p> <p>OF</p>	
	<p>FOR:</p> <p><b>HAAGEN COMPANY, LLC</b></p>	<p>SP-1</p>	<p>SP-2</p>
	<p>DATE:</p> <p>10/20/2018</p>	<p>BY:</p> <p>[Signature]</p>	<p>SCALE:</p> <p>AS SHOWN</p>

Project No. 0001 - 2020  
 Date: 01/20/2020  
 Scale: 1" = 40' (AS SHOWN)  
 Author: J. [unreadable]  
 Checker: J. [unreadable]  
 Date: 01/20/2020

SEE SHEET SP-1

CYMO STORM WATER CHANNEL



LEGEND	
	RIGHT OF WAY LINES
	CONCRETE
	EXISTING CONTOURS
	EXISTING OVERHEAD TELEPHONE
	EXISTING OVERHEAD ELECTRIC
	EXISTING PRIVATE DRIVE T&E LINE
	EXISTING WATER LINE
	EXISTING IRRIGATION LINE
	RETAIL BUILDING
	FUEL CENTER
	SMALL BUSINESS
	LARGE WAREHOUSE
	SMALL WAREHOUSE
	BRICK YARD
	PROPOSED CONCRETE SUBBASE
	PROPOSED AC PAVEMENT
	LOT NUMBER
	PARKING SPACES

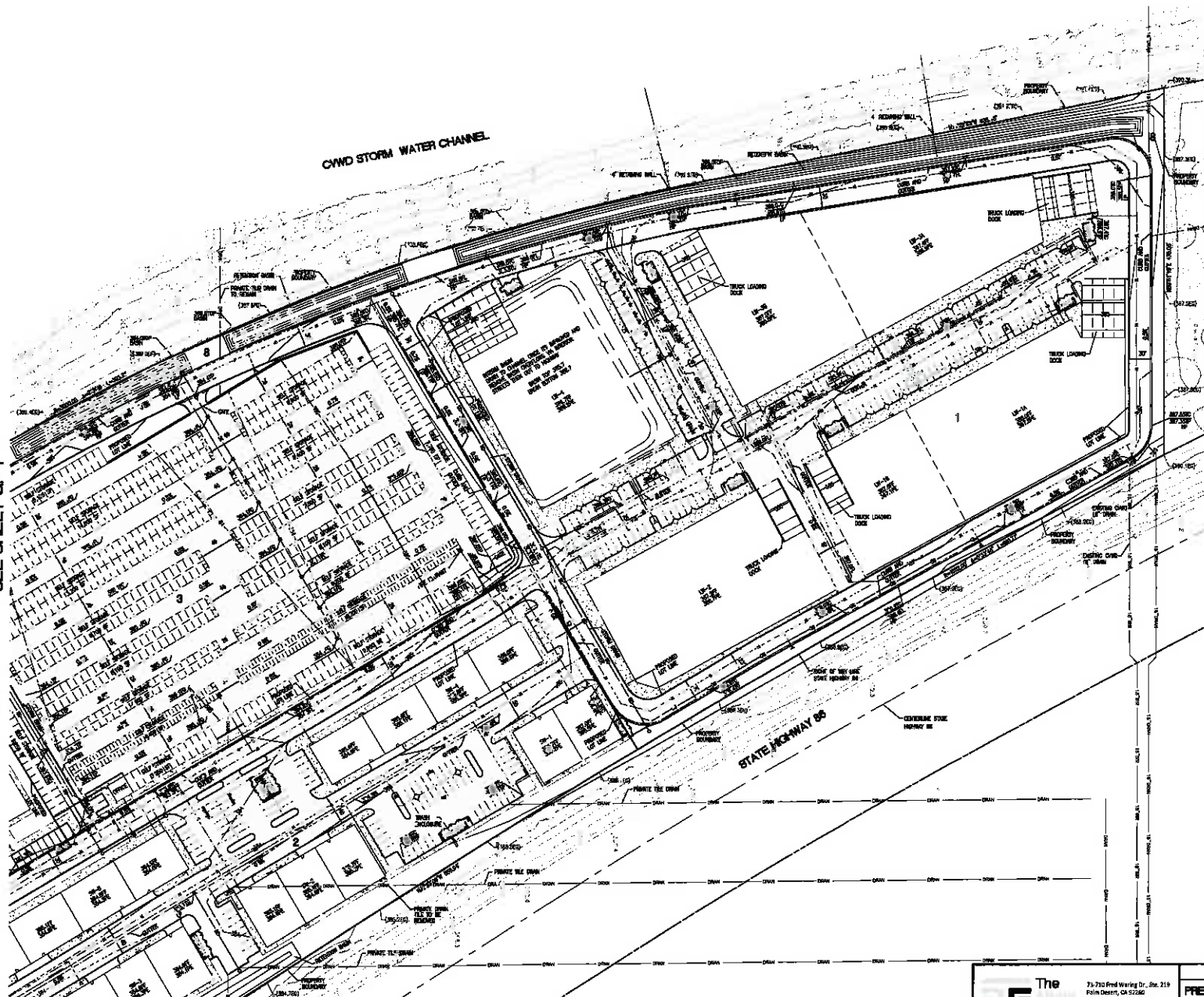
78-710 Peel Waring Dr., Ste. 218  
 Palm Beach, CA 92260  
 (714) 848-1700 | (714) 848-2019  
 TheGroupInc.com  
 ENGINEERING | PLANNING | SURVEY | ENVIRONMENTAL

CITY OF COACHELLA, CA  
**PRELIMINARY SITE PLAN**  
**COACHELLA AIRPORT BUSINESS PARK**  
 BEING A PORTION OF THE SOUTH HALF OF SECTION 15, T8S, R9E, S9M  
 FOR: **HAAGEN COMPANY, LLC**

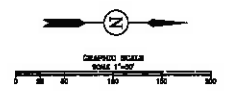
SHEET NO. **SP-2**  
 OF **SP-2**

DATE: 11-16-2010 10:43:10 AM C:\Users\jhaug\Documents\Projects\Coachella Airport Business Park\GP-1.dwg  
 PLOT DATE: Jul 08, 2008 10:58 AM  
 PLOT BY: jhaug

SEE SHEET GP-1



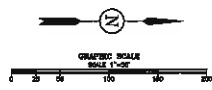
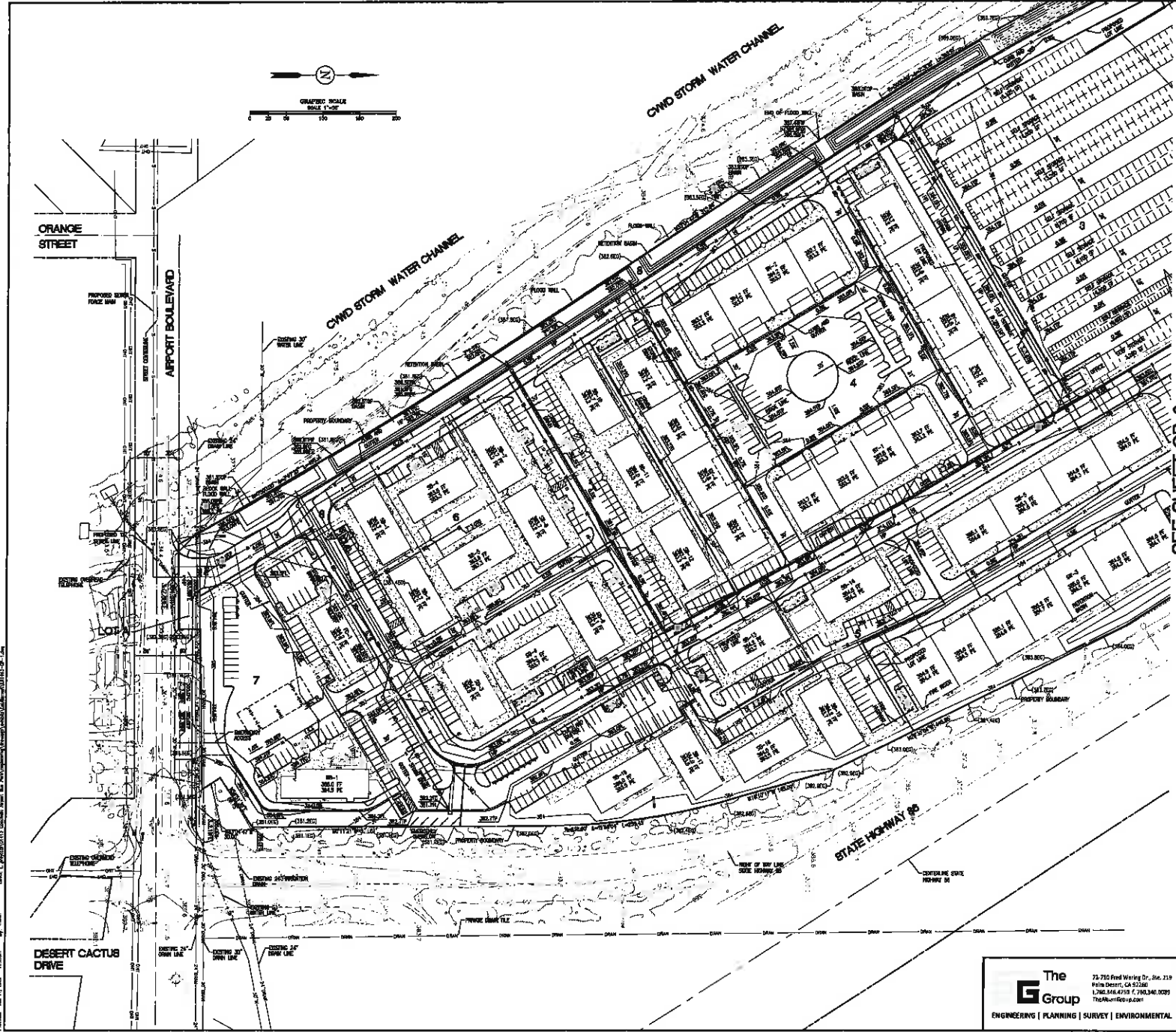
LEGEND	
	RIGHT OF WAY LINE
	CENTERLINE
	EXISTING CONTOURS
	EXISTING OVERHEAD TELEPHONE
	EXISTING UNDERGROUND ELECTRIC
	EXISTING PRIVATE SEWER TIE LINE
	EXISTING WATER LINE
	EXISTING PROTECTION LINE
	PROPOSED WATER LINE
	PROPOSED SEWER LINE
	PROPOSED STORM WATER LINE
	SEWAL BUILDING
	FUEL CENTER
	SEWAL WAREHOUSE
	LARGE WAREHOUSE
	SMALL WAREHOUSE
	BRICK YARD
	PAVED PLAZA
	PAVED ELEVATOR
	TOP OF PAVEMENT
	TOP OF DRIVE
	PROPOSED SURFACE
	FLOOR LINE
	TOP OF GARAGE
	GARAGE FLOOR
	HIGH POINT
	LOW POINT
	EXISTING GRADE
	PROPOSED CONCRETE SIDEWALK
	PROPOSED AC PAVEMENT
	LOT NUMBER
	PROPOSED FIRE HYDRANT
	PROPOSED WATER METER



The Group  
 ENGINEERING | PLANNING | SURVEY | ENVIRONMENTAL  
 7370 Fred Waring Dr., Ste. 219  
 Palm Desert, CA 92260  
 1.762.345.4710 • FAX 1.762.345.0097  
 TheGroupInc.com

CITY OF COACHELLA, CA  
**PRELIMINARY GRADING AND DRAINAGE EXHIBIT**  
**COACHELLA AIRPORT BUSINESS PARK**  
 BEING A PORTION OF THE SOUTH HALF OF SECTION 13, T8S, 18E, 36M  
 FOR: HAAGEN COMPANY, LLC

SHEET NO.  
 GP-1  
 OF  
 GP-2



**LEGEND**

---	EDGE OF TYP LINES
---	CONTOUR
---	EXISTING CONTOUR
---	EXISTING OVERHEAD TELEPHONE
---	EXISTING UNDERGROUND ELECTRIC
---	EXISTING PRIVATE DRAIN TIE LINE
---	EXISTING WATER LINE
---	EXISTING WINDROCK LINE
---	PROPOSED WATER LINE
---	PROPOSED SEWER LINE
---	PROPOSED RETURN DRAIN MAIN
---	REAR BUILDING
---	FUEL TOWER
---	SMALL BUILDING
---	LARGE WAREHOUSE
---	SKULL WAREHOUSE
---	BRICK WARE
---	FINISHED FLOOR
---	FIN. ELEVATION
---	TOP OF FINISH
---	TOP OF CURB
---	FINISHED SURFACE
---	FLOOR LINE
---	TOP OF DRIVE
---	ORANGE DRIVE
---	HOOT POOL
---	LOW POINT
---	EXISTING GRADE
---	PROPOSED CONCRETE SIDEWALK
---	PROPOSED AC PAVEMENT
---	LOT NUMBER
---	PROPOSED FIRE MANSION
---	PROPOSED WATER MANSION

SEE SHEET GP-2

**PRELIMINARY EARTHWORK VOLUMES**

(INCLUDES KNOWN FROM)

	CUT	FILL
SWP	34,888 CU FT.	26,274 CU FT.
SEWERAGE (10')		6,994 CU FT.
SEWERAGE (18")		3,028 CU FT.
ROADWAY CROWN (10')		13,843 CU FT.
PAVEMENT OVER (18")		12,540 CU FT.
<b>TOTAL:</b>	<b>34,888 CU FT.</b>	<b>62,679 CU FT. MINUS</b>

**NOTE:**  
THE ARCHITECT MAKES NO REPRESENTATION CONCERNING THE QUANTITY VOLUMES QUANTITIES LISTED ABOVE. THE ENGINEER SHALL NOT BE RESPONSIBLE FOR ANY DISCREPANCY BETWEEN THE CALCULATED QUANTITIES AND THE ACTUAL QUANTITIES AT THE TIME OF CONSTRUCTION. THE CONTRACTOR IS RESPONSIBLE TO PERFORM HIS/HER OWN DUE DILIGENCE TO OBTAIN THE CORRECT QUANTITIES.

DATE: JAN 11, 2024 11:58 AM; PROJECT: 2400000101; SHEET: GP-2; DRAWING: PRELIMINARY EARTHWORK VOLUMES; SCALE: AS SHOWN; DESIGNER: HAAGEN COMPANY, L.L.C.; CHECKER: J. HARRIS; APPROVER: J. HARRIS; PLOT DATE: 1/11/2024 11:58 AM; PLOT SCALE: 1/8" = 1'-0"; PLOT SHEET: GP-2

**The Group**  
 73,710 Fred Waring Dr., Ste. 219  
 Palm Desert, CA 92260  
 1.800.446.4718 | 760.334.9070  
 TheGroup.com

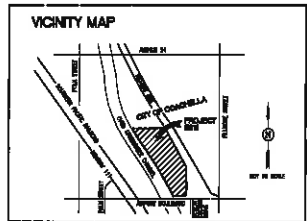
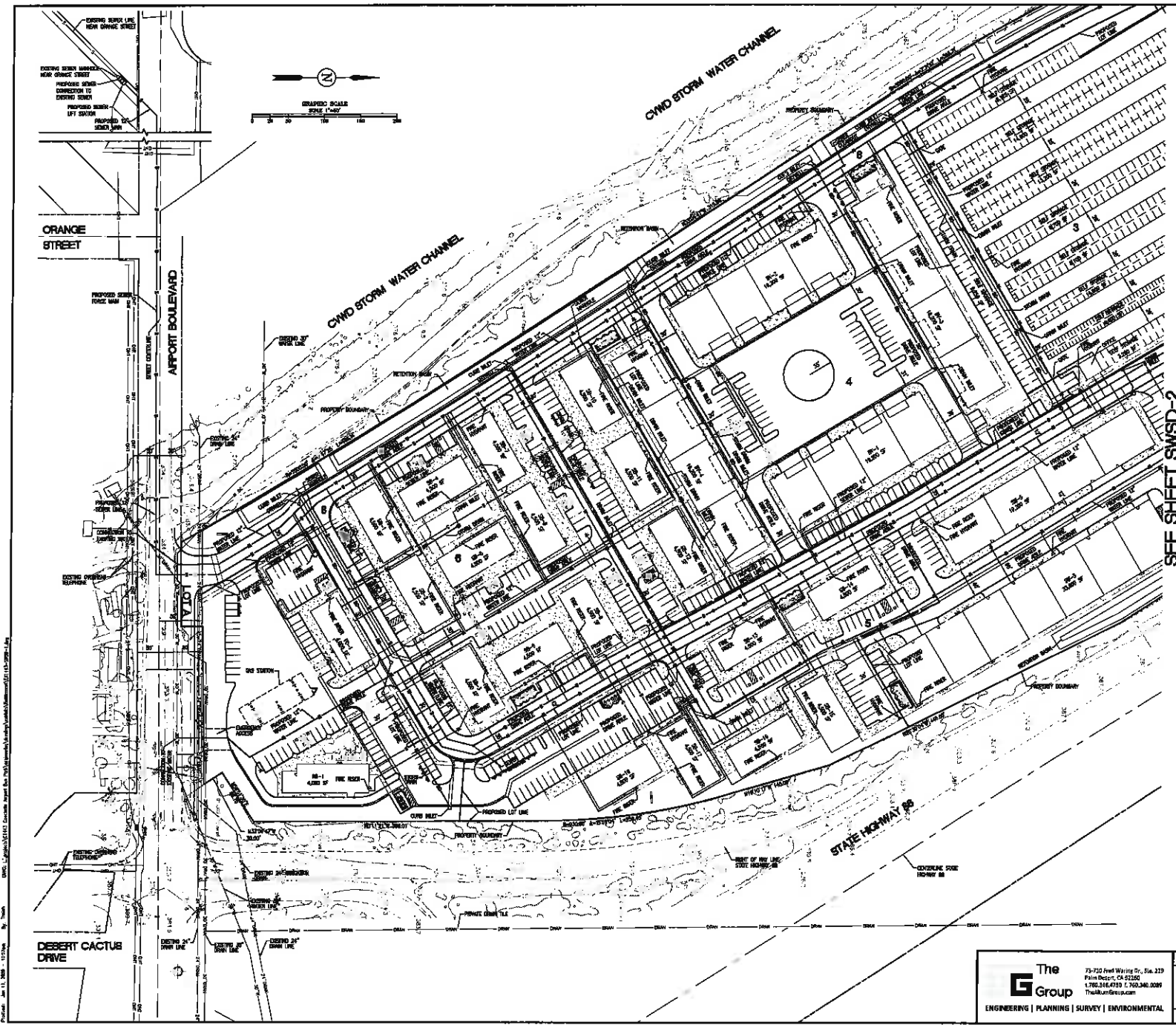
ENGINEERING | PLANNING | SURVEY | ENVIRONMENTAL

**CITY OF COACHELLA, CA**  
**PRELIMINARY GRADING AND DRAINAGE EXHIBIT**  
**COACHELLA AIRPORT BUSINESS PARK**

BEING A PART OF THE SOUTH HALF OF SECTION 15, T16S, R10E, S04

FOR: **HAAGEN COMPANY, L.L.C.**

SHEET NO.  
GP-1  
OF  
GP-2



**LEGEND**

---	ROOF OF NEW LINES
---	EXISTING
---	EXISTING CONDUIT
---	EXISTING OVERHEAD TELEPHONE
---	EXISTING UNDERGROUND ELECTRIC
---	EXISTING PRIVATE IRRAWI USE LINE
---	EXISTING WATER LINE
---	EXISTING IRRIGATION LINE
---	PROPOSED WATER LINE
---	PROPOSED SEWER LINE
---	PROPOSED STORM DRAIN MAN
---	NEW BUILDING
---	NEW CONCRETE
---	SMALL BUILDING
---	LARGE WAREHOUSE
---	SMALL WAREHOUSE
---	EXISTING CONCRETE SIDEWALK
---	PROPOSED AC PARKING
---	LOT NUMBER
---	PROPOSED FIRE HYDRANT
---	PROPOSED WATER METERS

SEE SHEET SWSD-2

DATE: 01/11/2018 10:53 AM BY: TMM

**The Group**  
 73-752 Ford Wayne Dr., Ste. 213  
 Palm Desert, CA 92260  
 1.762.346.6930 | 762.346.0099  
 TheGroup.com  
 ENGINEERING | PLANNING | SURVEY | ENVIRONMENTAL

**CITY OF COACHELLA, CA**  
**PRELIMINARY SEWER, WATER**  
**AND STORM DRAIN EXHIBIT**  
**COACHELLA AIRPORT BUSINESS PARK**  
BEING A PORTION OF THE SOUTH HALF OF SECTION 15, T10S, R10E, S04  
 FOR: **HAAGEN COMPANY, LLC**

SHEET NO. **SWSD-1**  
 OF **SWSD-2**

SEE SHEET SWSD-1

CWSD STORM WATER CHANNEL

STATE HIGHWAY 99

LEGEND	
	RIGHT OF WAY LINE
	CENTERLINE
	EXISTING CONTOURS
	EXISTING OVERHEAD TELEPHONE
	EXISTING UNDERGROUND ELECTRIC
	EXISTING PRIVATE DRIVE PILE LINE
	EXISTING METER LINE
	EXISTING MANHOLE LINE
	PROPOSED METER LINE
	PROPOSED SEWER LINE
	PROPOSED STORM DRAIN MAN
	ROAD BUILDING
	FUEL CENTER
	SMALL BUSINESS
	LARGE WAREHOUSE
	SMALL WAREHOUSE
	TRUCK YARD
	PROPOSED CONCRETE STRUCTURE
	PROPOSED AS PAVED
	LOT NUMBER
	PROPOSED FIRE HYDRANT
	PROPOSED WATER METER

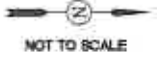


**The Group**  
 73-710 Fred Waring Dr., Ste. 218  
 Palm Desert, CA 92260  
 970.344.4700 • 762.343.0200  
 TheGroup@aig.com  
 ENGINEERING | PLANNING | SURVEY | ENVIRONMENTAL

CITY OF COACHELLA, CA  
 PRELIMINARY SEWER WATER  
 AND STORM DRAIN EXHIBIT  
**COACHELLA AIRPORT BUSINESS PARK**  
 BEING A PORTION OF THE SOUTH HALF OF SECTION 10, T4S, R9E, S24E  
 FOR: **HAAGEN COMPANY, LLC**

SHEET NO.  
**SWSD-2**  
 OF  
**SWSD-2**

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LEGEND	
	PHASE 1
	PHASE 2
	PHASE 3
	PHASE 1 BOUNDARY
	PHASE 2 BOUNDARY
	PHASE 3 BOUNDARY

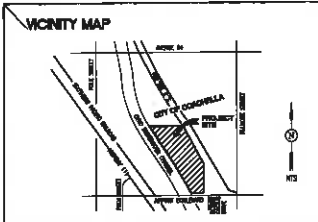
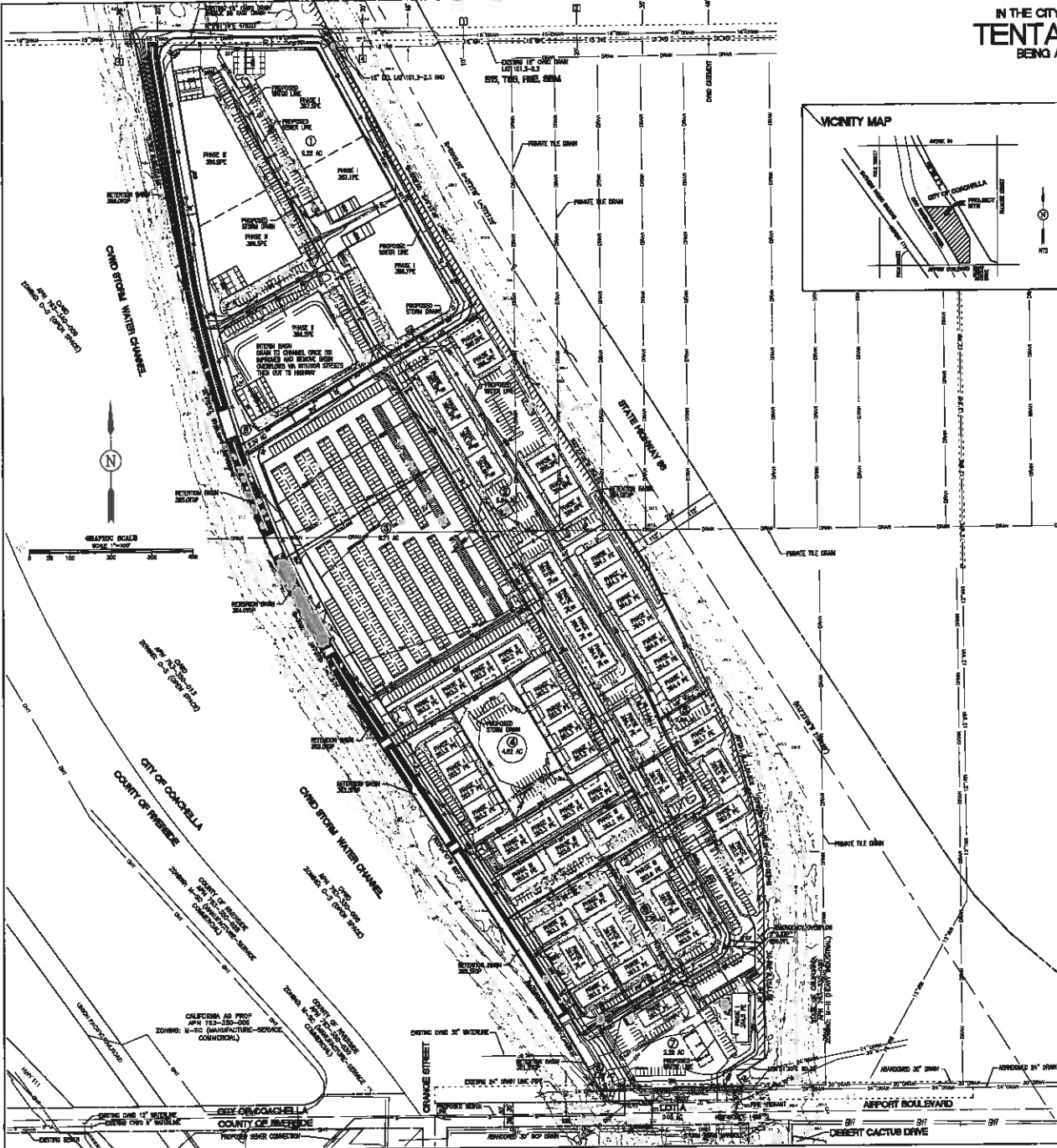
**The Altum Group**  
 ENGINEERING | PLANNING | SURVEY | ENVIRONMENTAL

CITY OF COACHELLA, CA  
 PHASING EXHIBIT  
 COACHELLA AIRPORT BUSINESS PARK  
 HAAGEN COMPANY, LLC

PP-1  
 PP-1



IN THE CITY OF COACHELLA, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA  
**TENTATIVE PARCEL MAP NO. 39721**  
 BEING A PORTION OF THE SOUTH HALF OF SECTION 15, T06, R08E, S04M  
 PARCELS 5 AND 6 FOR CONDOMINIUM PURPOSES  
 DATE: JUNE, 2020



**GENERAL NOTES**

1. TOTAL AREA - 48.89 ACRES, OVER.
2. SURVEY CONTROL AND WATER SUPPLY TO BE PROVIDED BY COACHELLA VALLEY WATER DISTRICT.
3. FLOOD HAZARD ZONE - FLOOD ZONE AS PER SPECIAL FLOOD HAZARD MAPS WITH WAVE FLOOD ELEVATION (WFE).
4. FLOODPLAIN PER FEMA MAP NO. 48002C00000E DATE 08/08/2018.
5. ADJACENT COMMUNITY ZONES, ADJACENT ZONING, SEE 0.
6. DRAINAGE DESIGN TO SUPPORT SEWER MAY REQUIRE SPECIALIZED DRAINAGE PROFESSIONAL DESIGNER IN ACCORDANCE WITH COACHELLA WATER WASTEWATER POLICY DIRECTIVE NO. 13-02, INTERSECTION CONTROL, CALCULATION AND CHANGES PER BOXES 40.1 AND PER PERFORM COMPLETE STREETS INTERSECTION GUIDE, TOWNSHIP AND ALL APPLICABLE ORDINANCES.
7. PARCELS 5 AND 6 ARE FOR CONDOMINIUM PURPOSES.

**AREA TABLE**

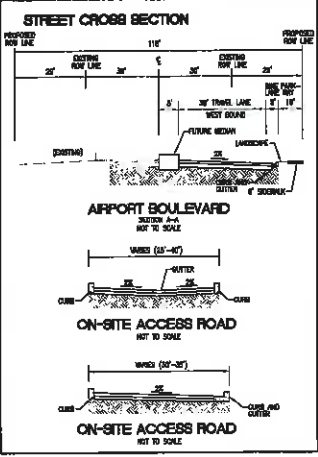
PARCEL NUMBER	AREA	PARCEL NUMBER	AREA
1	0.23 AC	6	2.76 AC
2	1.08 AC	7	2.82 AC
3	0.77 AC	8	0.36 AC
4	4.62 AC	9	0.98 AC
5	2.84 AC		
TOTAL AREA: 48.89 AC			

**LEGEND**

- PROJECT BOUNDARY
- ROOF OF WAY LINES
- RECORDING ACCESS
- LOT LINES
- PROPOSED WALK LANE
- PROPOSED WATER LANE
- PROPOSED SEWER LANE
- PROPOSED STORM DRAIN
- PARCEL NUMBER

**EXISTING EASEMENTS**

1. 10' WIDE PIPELINE EASEMENT IN FAVOR OF THE UNITED STATES OF AMERICA RECORDED 09/30/1946 IN BOOK 1018, PAGE 377 OF OFFICIAL RECORDS.
2. 20' WIDE ROAD EASEMENT IN FAVOR OF THE SOUTHERN PACIFIC LAND COMPANY RECORDED 01/27/1924 IN BOOK 1946, PAGE 33 OF OFFICIAL RECORDS.
3. 40' WIDE PIPELINE EASEMENT IN FAVOR OF THE COACHELLA VALLEY COUNTY WATER DISTRICT RECORDED 04/11/1955 AS INST. NO. 23579 OF OFFICIAL RECORDS.
4. 20' WIDE PIPELINE EASEMENT IN FAVOR OF THE COACHELLA VALLEY COUNTY WATER DISTRICT RECORDED 08/21/1983 AS INST. NO. 58875 OF OFFICIAL RECORDS.
5. PROPOSED NO-BUILT EASEMENT.



**DEVELOPER**

AVIATE AIRPORT, LLC BY AVIATE COMPANY  
 GEORGE OWEN FINEY, PRESIDENT AND CHIEF OPERATING OFFICER  
 12301 AIRPORT BOULEVARD  
 LOS ANGELES, CA 90044  
 TEL: (310) 884-1200  
 EMAIL: GOWEN@AVIATE.COM

**OWNER**

AVIATE AIRPORT, LLC BY AVIATE COMPANY  
 GEORGE OWEN FINEY, PRESIDENT AND CHIEF OPERATING OFFICER  
 12301 AIRPORT BOULEVARD  
 LOS ANGELES, CA 90044  
 TEL: (310) 884-1200  
 FOR: EMAIL: GOWEN@AVIATE.COM

**ENGINEER**

THE ALTAIR GROUP  
 73-719 Foothill Parkway, Suite 210  
 Fullerton, CA 92631  
 TEL: (714) 346-4700

**LEGAL DESCRIPTION**

BEING A PORTION OF THE SOUTHWEST QUARTER AND THE SOUTHWEST QUARTER OF SECTION 15, TOWNSHIP 6, RANGE 8 EAST, AND RANGES 8 EAST AND 9 EAST, IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, ACCORDING TO THE OFFICIAL PLAT, THEREOF.

**AFN**

763-330-410, 763-330-418 & 763-330-420

**ZONING/GENERAL PLAN**

DISTRICT ZONING: M-1 (MEDIUM DENSITY)  
 PROPOSED ZONING: M-2 (MANUFACTURING SERVICE) WITH A-1 (INDUSTRIAL PARK) OVERLAY  
 GENERAL PLAN: L (LOW DENSITY)  
 EXISTING USE: VACANT  
 PROPOSED USE: LARGE & SMALL WAREHOUSE, SMALL BUSINESS, SELF STORAGE, SERVICE STORAGE, OFFICE SPACE, AND RETAIL (SEE STORES AND DRIVE DRIVELANDS)

**LOT REQUIREMENTS**

6' DENSITY  
 MINIMUM LOT SIZE: MINIMUM LINES - 10 FEET, ALL OTHER LINES - 1 FIVE FEET  
 MINIMUM LOT WIDTH: 100 FEET  
 MINIMUM LOT DEPTH: 200 FEET  
 MINIMUM LOT COVERAGE: 5%

**YARD REQUIREMENTS**

1. THERE IS NO 4-6 ZONE ADJACENT TO A STREET OR HIGHWAY WHERE TWO-THIRDS OF THE PROPERTY IS IN THE BACK ON THE OPPOSITE SIDE OF THE STREET IN ZONING RECORDS. A YARD SHALL BE PROVIDED OF TEN (10) FEET IN DEPTH.
2. THERE IS NO 4-6 ZONE ADJACENT TO A STREET OR HIGHWAY AND THERE ARE NO OTHER ADJACENT ZONES AND RECORDS. A YARD SHALL BE PROVIDED WHICH IS EQUAL IN DEPTH TO THE DEPTH OF THE RECORDS OF THE ADJACENT ZONES.
3. THERE IS NO 4-6 ZONE ADJACENT TO A STREET OR HIGHWAY AS SHOWN ON THE CALCULATED ELEVATION OF THE GENERAL PLAN. A YARD SHALL BE PROVIDED OF TEN (10) FEET IN DEPTH.
4. THERE IS NO 4-6 ZONE ADJACENT TO A RECORDING ZONE. THERE SHALL BE A YARD OF NOT LESS THAN TEN (10) FEET ADJACENT TO THE ZONE. THIS SHALL BE REQUIRED TO MEET (10) FEET WHERE THERE EXISTS A PUBLIC ALLEY ADJACENT TO THE TWO ZONES.

**BASIS OF BEARINGS**

CALIFORNIA COORDINATE SYSTEM, ZONE 6, NAD 83 (NAD 83) EPOCH BETWEEN CITY OF COACHELLA RM 1014 AND RM 1010 TAKEN AS 802449197 FOR THE CITY OF COACHELLA BENCHMARK BOOK.

**BENCH MARK**

CITY OF COACHELLA RM 1010:  
 3-1/2" BRASS DISK SET IN THE TOP OF CURB ON THE SOUTH SIDE OF AIRPORT BOULEVARD. IT IS 2' SOUTH OF THE CENTERLINE OF AIRPORT BOULEVARD AND 2' EAST OF THE SOUTHWEST CORNER OF THE BRIDGE OVER RAY AVENUE S.

ELEVATION = 407.665 MVD 88  
 THE ELEVATION LISTED ABOVE IS ACTUAL ELEVATION + 800' FEET TO AVOID NEGATIVE ELEVATIONS.

**AERIAL TOPOGRAPHIC**

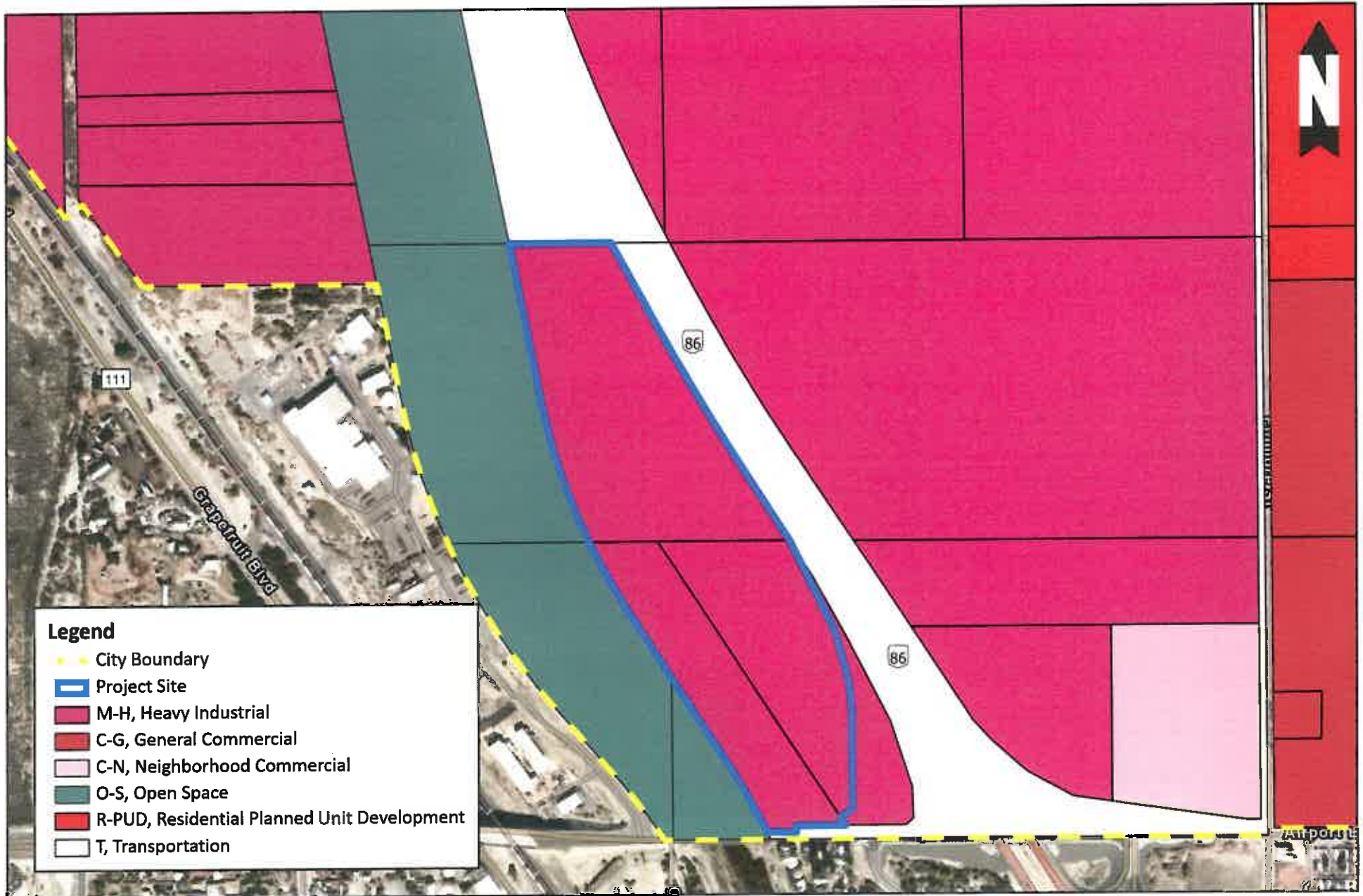
**AEROTECH MAPPING, INC.**  
 28970 TECHNOLOGY DRIVE, SUITE 220-C  
 BARRINGTON, CA 94503 | (925) 806-5000  
 AERIAL PHOTOGRAPHY DATED: 4-11-2018

**PREPARED BY:**

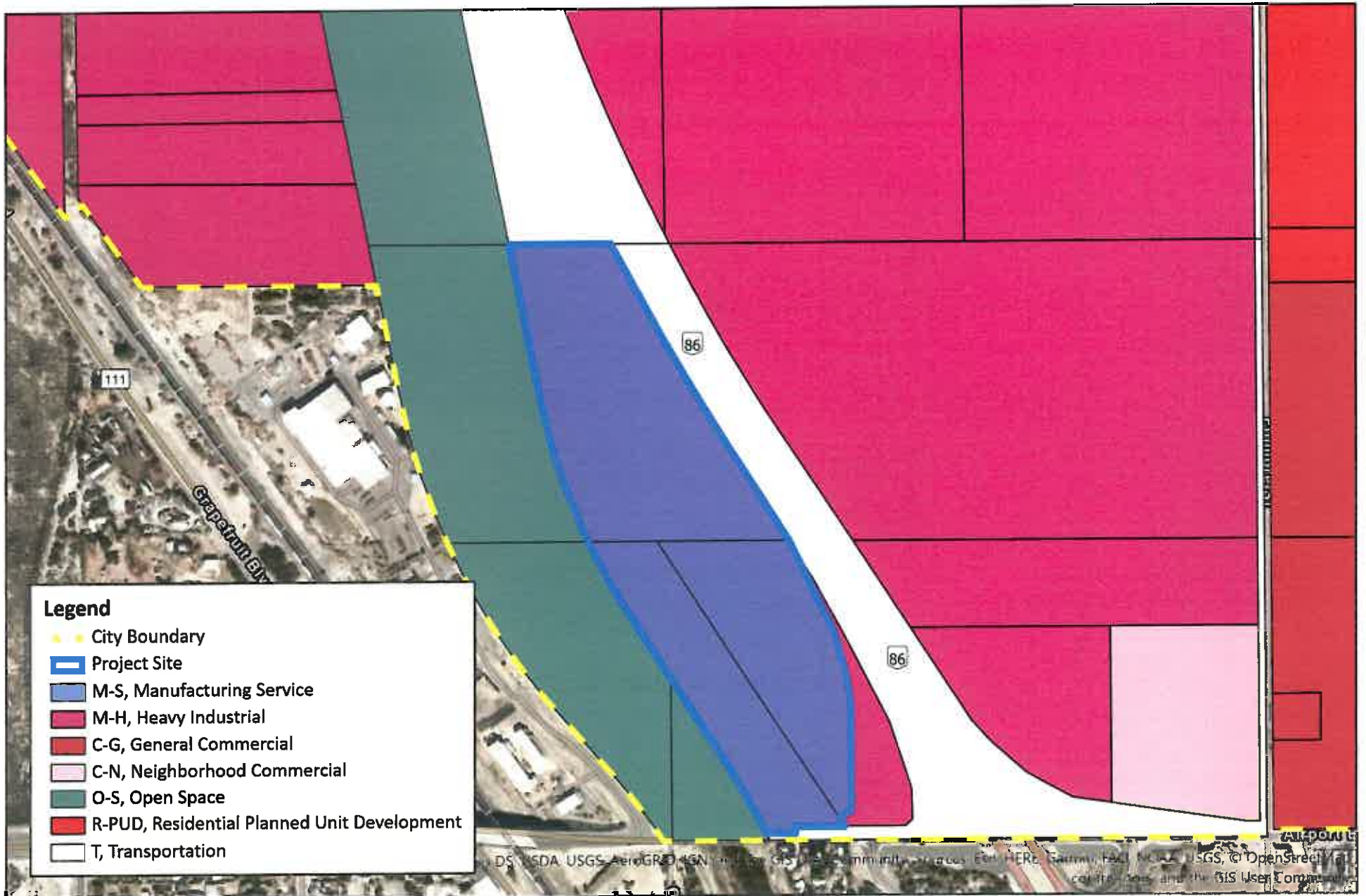
**IG** Engineering • Planning  
 Survey • Environmental

The Altair Group  
 73-719 Foothill Parkway, Suite 210, Fullerton, CA 92630  
 1.769.346.4700 TheAltairGroup.com | 714.346.4700

**SHEET NO.**  
 1  
 OF 1



1 IN = 600 FT



1 IN = 600 FT



1 IN = 800 FT



1 IN = 800 FT

**NOTICE OF PUBLIC HEARING**  
**RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**  
[www.rcaluc.org](http://www.rcaluc.org)

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893**. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The City of Coachella should be contacted on non-ALUC issues. For more information please contact City of Coachella Planner Mr. Luis Lopez at (760) 398-3102.

The proposed project application may be viewed at [www.rcaluc.org](http://www.rcaluc.org). Written comments may be submitted to the Riverside County ALUC by e-mail to [prull@rivco.org](mailto:prull@rivco.org), or by U.S. mail to Riverside County Administrative Center, 4080 Lemon Street, 14<sup>th</sup> Floor, Riverside, California 92501. Individuals with disabilities requiring reasonable modifications or accommodations, please telephone Barbara Santos at (951) 955-5132.

**PLACE OF HEARING:** Riverside County Administration Center  
4080 Lemon Street, 1<sup>st</sup> Floor Board Chambers  
Riverside California

**DATE OF HEARING:** August 13, 2020

**TIME OF HEARING:** 9:30 A.M.

**Pursuant to Executive Order N-25-20, this meeting will be conducted by teleconference only. Information on how to participate in the hearing will be available on the ALUC website at [www.rcaluc.org](http://www.rcaluc.org)**

**CASE DESCRIPTION:**

ZAP1050TH20 – Empire Airport, LLC/Hagen Company, LLC (Representative: The Altum Group) – City of Coachella Planning Case Nos. GPA20-01 (General Plan Amendment), CZ20-01 (Change of Zone), CUP324 CUP325 CUP326 (Conditional Use Permits), AR20-04 (Architectural Review), PM39721 (Tentative Parcel Map). The applicant proposes to develop a 629,450 square foot mixed-use business park center on 42.36 acres including 233,100 square feet of large warehouse buildings, 96,000 square feet of small warehouse manufacturing buildings, 81,000 square feet of small business manufacturing buildings, 133,900 square foot self-storage facility, 76,800 square foot personal vehicle storage garage, a 4,000 square foot gas station convenience store with 10 fueling stations, and a 4,650 square foot restaurant with 9 car stack drive-thru, located northerly of Airport Boulevard, southerly of 55<sup>th</sup> Avenue, westerly of State Route 86, and easterly of the Coachella Valley Water District Storm Water Channel. The applicant also proposes amending the site's General Plan land use designation from Sub-Area 8 into Sub-Area 7, to allow for commercial cannabis-related uses, and changing the site's zoning from Heavy Industrial (M-H) to Manufacturing Service (M-S). A tentative parcel map is also proposed to divide the site into 7 commercial parcels (Airport Compatibility Zone D of the Jacqueline Cochran Regional Airport Influence Area)



# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

JCRA  
ZONE  
D

## APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP 1050 TH 20 DATE SUBMITTED: 6-30-20

### APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

Applicant	Empire Airport, LLC by Hagen Company, LLC	Phone Number	310-820-1200
Mailing Address	Christopher Fahey, President and COO 12302 Exposition Blvd; Los Angeles, CA 90064	Email	cfahey@haagenco.com
Representative	The Altum Group - Rich Malacoff, AICP	Phone Number	760-641-3065
Mailing Address	73710 Fred Waring Drive	Email	rich.malacoff@thealtumgroup.com
Property Owner	Empire Airport, LLC by Hagen Company, LLC	Phone Number	310-820-1200
Mailing Address	Christopher Fahey, President and COO 12302 Exposition Blvd; Los Angeles, CA 90064	Email	cfahey@haagenco.com

### LOCAL JURISDICTION AGENCY

Local Agency Name	City of Coachella	Phone Number	760-398-3102
Staff Contact	Luis Lopez, Community Development Director	Email	llopez@coachella.org
Mailing Address	1515 Sixth Street Coachella, CA 92236	Case Type	<input checked="" type="checkbox"/> General Plan / Specific Plan Amendment <input type="checkbox"/> Zoning Ordinance Amendment <input type="checkbox"/> Subdivision Parcel Map / Tentative Tract <input checked="" type="checkbox"/> Use Permit (3 Conditional Use Permits) <input checked="" type="checkbox"/> Site Plan Review/Plot Plan <input type="checkbox"/> Other
Local Agency Project No	AR: 20-04 / <u>PM 39721</u>		

### PROJECT LOCATION

*Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways*

Street Address	Northwest corner of the intersection of Airport Blvd and Hwy 86		
Assessor's Parcel No.	APNs 763-330-013, 018 & 029 for Project Site	Gross Parcel Size	42.69 acres
Subdivision Name	763-340-009, 763-350-013, and 763-330-006 are for City Initiated General Plan Amendment	Nearest Airport and distance from Airport	1 Mile from Site
Lot Number	and are off-site.		J.C. Regional Airport

### PROJECT DESCRIPTION

*If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed*

Existing Land Use (describe)	Vacant Property

Proposed Land Use (describe)	The proposed project consists of phased development of a 628,825 sq. ft. commercial/industrial center, with owner occupied buildings on 42.69 acres of vacant land to include the following uses: a 233,100 sq. ft. Large Warehouse; an 96,000 sq. ft. Small Warehouse; an 81,000 sq. ft. Small Business building; a 76,800 sq. ft. storage building an 133,275 sq. ft. Self-Storage, a 4,000 sq. ft. Service Station/Mini Mart and a 4,650 sq. ft. Drive-Thru Restaurant; in th G-C (General Commercial) Zone located at the northwest corner of Airport Boulevard and California Expressway		
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)	N/A	
For Other Land Uses (See Appendix C)	Hours of Operation	24 Hours	
	Number of People on Site	500	Maximum Number 500
	Method of Calculation	Source: Socioeconomic Build-Out Assumptions and Methodology, County of Riverside, April 2017	
Height Data	Site Elevation (above mean sea level)	117 feet below Sea Level ft.	
	Height of buildings or structures (from the ground)	50 Feet ft.	
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?	<input type="checkbox"/> Yes	
		<input checked="" type="checkbox"/> No	
	If yes, describe	_____	
		_____	
		_____	

- A. **NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. **REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. **SUBMISSION PACKAGE:**
1. . . . . Completed ALUC Application Form #1
  1. . . . . ALUC fee payment #2 - Attached
  1. . . . . Plans Package (24x36 folded) (site plans, floor plans, building elevations, #3 grading plans, subdivision maps)
  1. . . . . Plans Package (8.5x11) (site plans, floor plans, building elevations, #4 grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
  1. . . . . CD with digital files of the plans (pdf) #5 - Thumb Drive
  1. . . . . Vicinity Map (8.5x11) #6
  1. . . . . Detailed project description #7
  1. . . . . Local jurisdiction project transmittal #8 (TBD)
  3. . . . . Gummed address labels for applicant/representative/property owner/local jurisdiction planner #9
  3. . . . . Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. (Only required if the project is scheduled for a public hearing Commission meeting)



# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

## STAFF REPORT

### ADMINISTRATIVE ITEMS

#### 4.1 Director's Approvals.

- A. During the period of June 16 through July 15, 2020, as authorized pursuant to Section 1.5.2(d) of the 2004 Riverside County Airport Land Use Compatibility Plan, ALUC Director Simon Housman reviewed one non-legislative case within Zone D of the Bermuda Dunes Airport Influence Area and issued a determination of consistency.

ZAP1082BD20 (Bermuda Dunes Airport Influence Area, Zone D) pertains to the County of Riverside Case No. CUP190040 (Conditional Use Permit), a proposal to establish a cannabis retail facility/dispensary with distribution and cultivation/manufacturing areas within an existing 6,000 square foot building on a 0.71 acre parcel located at 77704 Flora Road. The site is located within Airport Compatibility Zone D of the Bermuda Dunes Airport Influence Area ("March AIA"), where non-residential intensity is restricted to an average of 100 people per acre and a maximum of 300 people in any given single acre area. The proposed project, with 2,380 square feet of retail space, 1,435 square feet of storage/distribution area, and 1,400 square feet of manufacturing area, accommodates a total of 51 people, resulting in an average intensity of 72 persons per acre, which is consistent with the Zone D intensity criterion.

The elevation of Runway 10-28 at its westerly terminus is approximately 73 feet above mean sea level (AMSL). At a distance of approximately 9,700 feet from the runway, FAA review would be required for any structures with top of roof exceeding 170 feet AMSL. The site elevation is approximately 116 feet AMSL, with an existing building height of 28 feet, resulting in a top point elevation of 144 feet AMSL. No new buildings or structures are proposed. Therefore, review of buildings by the FAA Obstruction Evaluation Service is not required.

ALUC Director Simon Housman issued a determination of consistency for this project on June 29, 2020.

\*\*\*\*\*

- B. Additionally, ALUC Director Simon Housman reviewed one Citywide non-impact legislative case (ordinance amendments) pursuant to ALUC Resolution No. 2011-02 and issued a determination of consistency.

ZAP1052RG20 (City of Riverside – Citywide) pertains to City of Riverside Case Nos. P20-0179 (Zoning Ordinance Amendment), P20-0190 (Specific Plan Amendment), P20-0191 (Specific Plan Amendment). P20-0179 is a proposal to amend the City's Zoning Code (Title 19) implementing the policies of the revised Good Neighbor Guidelines for Siting New and/or Modified Industrial Facilities. Specifically, the amendment will incorporate these policies in the following sections: Base Zones and Related Use Development Provisions, Specific Land Use Provisions, Site Planning and General Development Provisions, and Definitions. The amendment includes the addition of a new chapter (19.435) providing updated development criteria for warehousing and distribution facilities. There are no additions to the permitted land use tables

and no development standards that would increase residential density or non-residential intensity.

ALUC Director Simon Housman issued a determination of consistency for this project on June 18, 2020

**4.2** Resolution No. 2020-01 Extending the Authorization of the ALUC Director to Take Action on Legislative Items in Zone E of the March Air Reserve Base/Inland Port Airport Influence Area.

On August 13, 2015, the ALUC adopted Resolution No. 2015-01 authorizing the ALUC Director to review general plan amendments, specific plan amendments, zone changes, and variance applications submitted to the ALUC for land use development review located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area pursuant to Public Utilities Code Section 21676 and to render a consistency determination as to any such application that is not inconsistent nor has the possibility to be inconsistent with the airport land use compatibility planning guidelines.

The resolution was previously extended via Resolution No. 2016-02 and Resolution No. 2018-02, ultimately extending the resolution until August 31, 2020.

Resolution No. 2020-01 seeks to extend this resolution once more until August 31, 2025.

**4.3** Update March ARB CUS

Presentation by ALUC Director Simon Housman or his designee.

**4.4** ALUC Website Update

Presentation by ALUC Director Simon Housman or his designee.

Y:\ALUC Administrative Items\Admin. 2020\ADmin Item 08-13-20.doc



**AIRPORT LAND USE COMMISSION  
RIVERSIDE COUNTY**

4.1

June 29, 2020

**CHAIR**  
Russell Betts  
Desert Hot Springs

**VICE CHAIR**  
Steven Stewart  
Palm Springs

**COMMISSIONERS**

Arthur Butler  
Riverside

John Lyon  
Riverside

Steve Manos  
Lake Elsinore

Richard Stewart  
Moreno Valley

Gary Youmans  
Temecula

**STAFF**

Director  
Simon A. Housman

John Guerin  
Paul Rull  
Barbara Santos

County Administrative Center  
4080 Lemon St., 14th Floor  
Riverside, CA 92501  
(951) 955-5132

[www.rcaluc.org](http://www.rcaluc.org)

Mr. Gabriel Villalobos, Project Planner  
Riverside County Planning Department  
4080 Lemon Street, 12<sup>th</sup> Floor  
Riverside CA 92501  
(VIA HAND DELIVERY)

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –  
DIRECTOR’S DETERMINATION**

File No.: ZAP1082BD20  
Related File No.: CUP190040 (Conditional Use Permit)  
APN: 626-420-025

Dear Mr. Villalobos:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC), staff reviewed Riverside County Case No. CUP190040 (Conditional Use Permit), a proposal to establish a cannabis retail facility/dispensary with distribution and cultivation/manufacturing areas within an existing 6,000 square foot building on a 0.71 acre parcel located at 77704 Flora Road (on the northerly side of Flora Road, easterly of El Viento Road, and westerly of Las Montanas Road and Washington Street) in the unincorporated area northwesterly of Bermuda Dunes Airport.

The site is located within Airport Compatibility Zone D of the Bermuda Dunes Airport Influence Area (AIA), which restricts non-residential intensity to an average of 100 people per acre and a maximum of 300 people in any given single-acre area (for properties of three or more acres). The proposed project, with 2,380 square feet of retail space, 1,435 square feet of storage/distribution area, and 1,400 square feet of manufacturing area, accommodates a total of 51 people, resulting in an average intensity of 72 persons per acre, which is consistent with the Zone D intensity criterion.

The elevation of Runway 10-28 at its westerly terminus is approximately 73 feet above mean sea level (AMSL). At a distance of approximately 9,700 feet from the runway, FAA review would be required for any structures with top of roof exceeding 170 feet AMSL. The site elevation is approximately 116 feet AMSL, with an existing building height of 28 feet, resulting in a top point elevation of 144 feet AMSL. No new buildings or structures are proposed. Therefore, review of buildings by the FAA Obstruction Evaluation Service is not required.

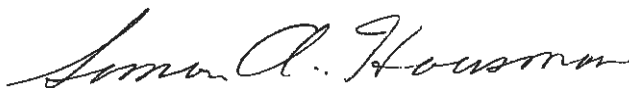
As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2004 Bermuda Dunes Airport Land Use Compatibility Plan, provided that the County of Riverside applies the following recommended conditions:

**CONDITIONS:**

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky, and shall comply with the requirements of Riverside County Ordinance No. 655, as applicable.
2. The following uses are not included in the project and shall be prohibited on this site:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area.
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Highly noise-sensitive outdoor nonresidential uses.
3. The attached notice shall be given to all prospective purchasers and/or tenants of the property, and shall be recorded as a deed notice.
4. Any ground-level or aboveground water detention basin or facilities shall be designed so as to provide for a detention period for the design storm that does not exceed 48 hours and to remain totally dry between rainfalls. Vegetation around such facilities that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature.

If you have any questions, please contact Paul Rull, ALUC Principal Planner, at (951) 955-6893.

Sincerely,  
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



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Simon A. Housman, ALUC Director

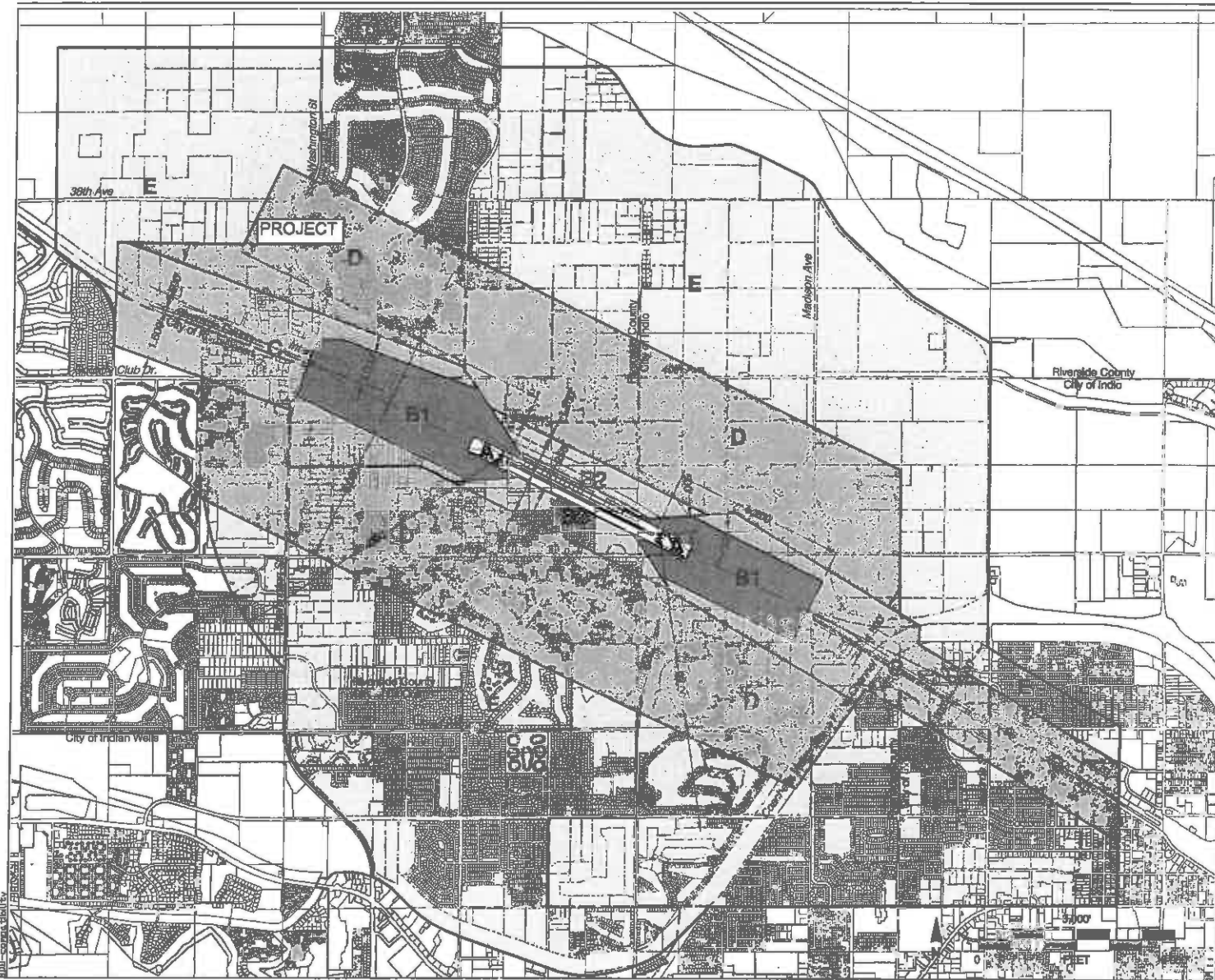
Attachments: Notice of Airport in Vicinity

cc: DJK Group, Joe Lizarraga (applicant/representative)  
Dave Boggs (property owner)  
Ann Goodwyn, Airport Manager, Bermuda Dunes Executive Airport  
ALUC Case File

Y:\AIRPORT CASE FILES\Bermuda Dunes\ZAP1082BD20\ZAP1082BD20.LTR.doc

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



**Legend**

**Compatibility Zones**

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E

**Boundary Lines**

- Airport Property Line
- - - City Limits

**Note**

Southwestern edge of Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

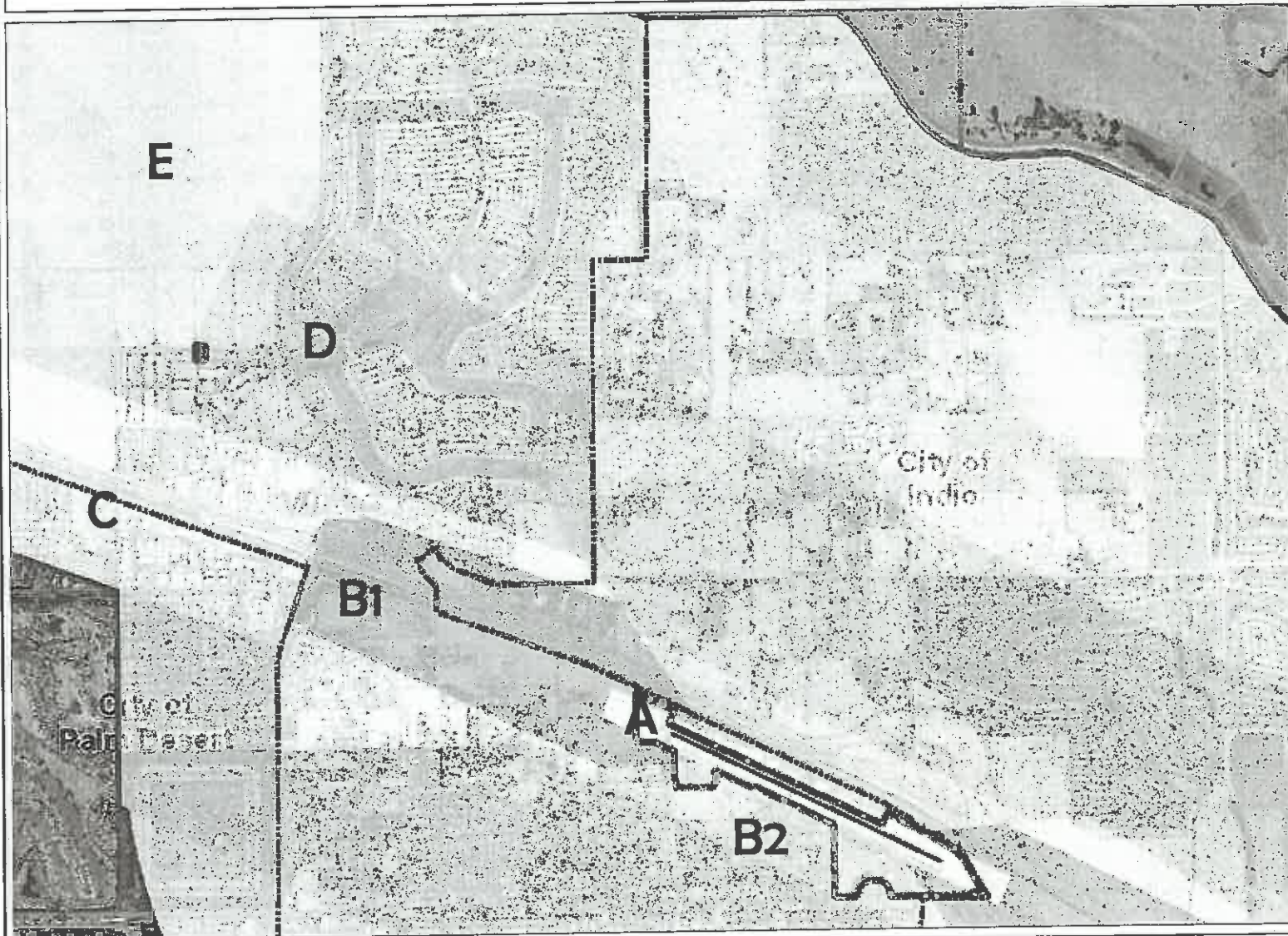
See Chapter 2, Table 2A for compatibility criteria associated with this map.

**Riverside County**  
**Airport Land Use Commission**  
**Riverside County**  
**Airport Land Use Compatibility Plan**  
**Policy Document**  
 (Adopted December 2004)

Map BD-1

**Compatibility Map**  
**Bermuda Dunes Airport**

# Map My County Map



## Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones
- ▨ OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6

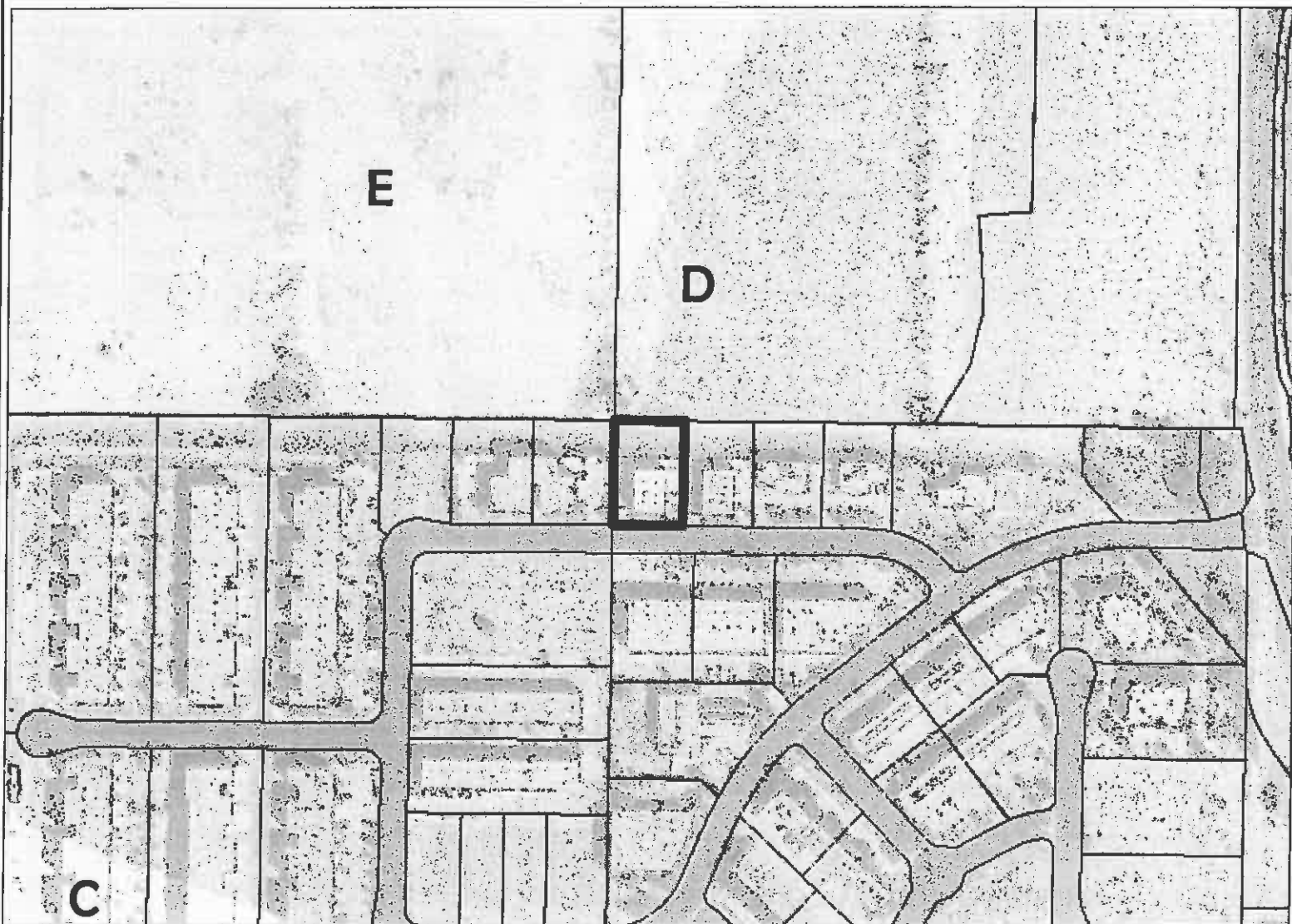
## Notes

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# Map My County Map



### Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
  - OTHER COMPATIBILITY ZONE
  - A
  - A-EXC1
  - B1
  - B1-APZ I
  - B1-APZ I-EXC1
  - B1-APZ II
  - B1-APZ II-EXC1
  - B1-EXC1
  - B2
  - B2-EXC1
  - C
  - C1
  - C1-EXC1
  - C1-EXC3
  - C1-EXC4
  - C1-HIGHT
  - C2
  - C2-EXC1
  - C2-EXC2
  - C2-EXC3
  - C2-EXC5

0 385 770 Feet

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### Notes

# Map My County Map

Los Angeles



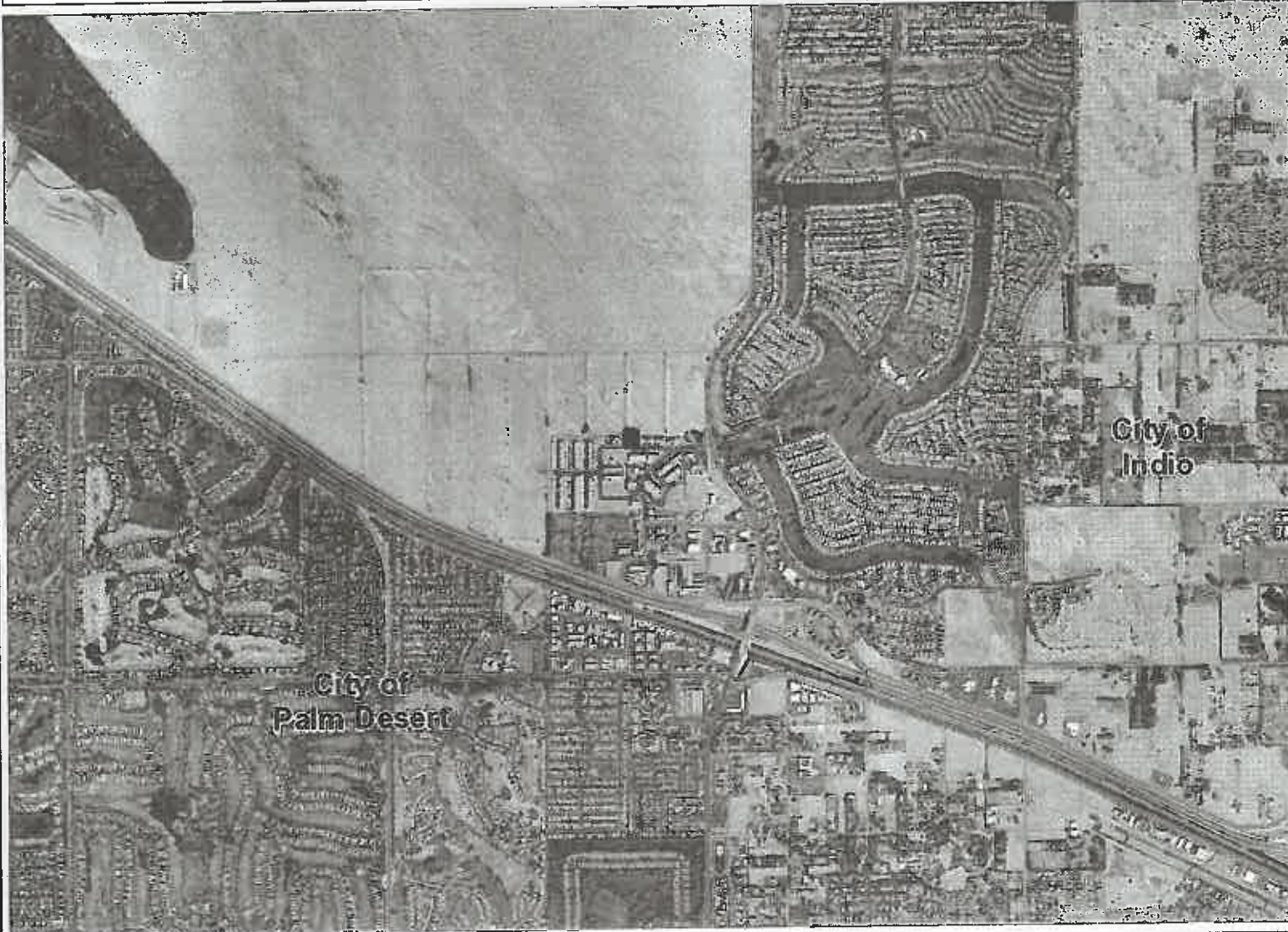
San Diego

Tijuana

Mexico

## Legend

- Blueline Streams
- City Areas
- World Street Map



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0 3,079 6,157 Feet

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## Notes

# Map My County Map



### Legend

- Blueline Streams
- City Areas
- World Street Map



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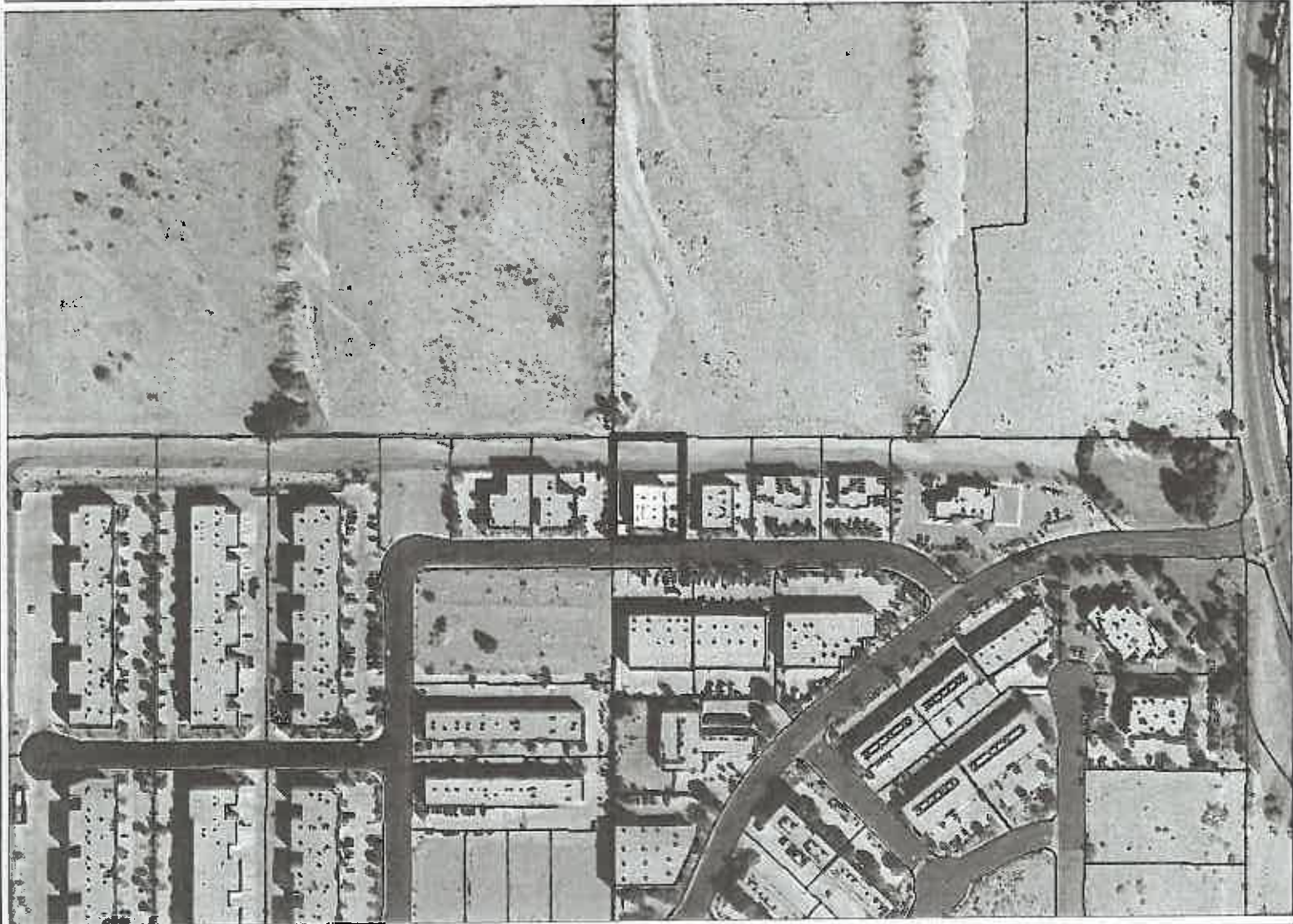
### Notes

# Map My County Map



## Legend

-  Parcels
-  Blueline Streams
-  City Areas
-  World Street Map



## Notes







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# Map My County Map



### Legend

-  Parcels
-  Blueline Streams
-  City Areas
-  World Street Map



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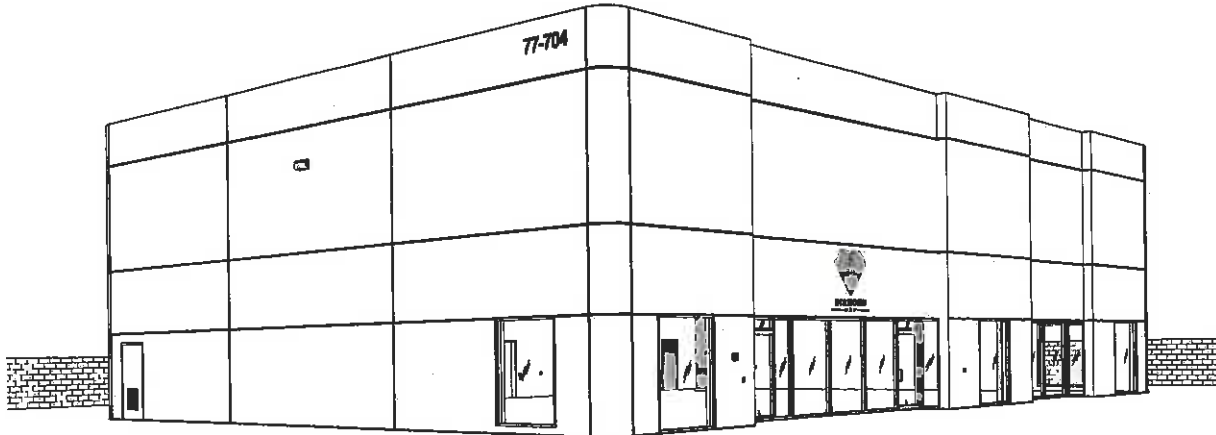
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© Riverside County GIS

### Notes

**DIAMOND SAP (DKJ GROUP)**  
**APPLICATION # CAN 190007**  
**77704 FLORA RD.**  
**PALM DESERT, CA 92211**



**SCHOOL DISTRICT:**  
 DESERT SANDS UNIFIED PH: (951) 777-4200

**UTILITY PURVEYORS:**  
**INTERNET / PHONE**  
 FRONTIER COMMUNICATIONS PH: (877) 708-8373

**CABLE**  
 SPECTRUM PH: (760) 874-4444

**WATER PROVIDER**  
 COACHELLA VALLEY WATER DISTRICT PH: (760) 369-2851

**SEWER**  
 COACHELLA VALLEY WATER DISTRICT PH: (760) 369-2851

**TRASH**  
 BURDETTE WASTE & RECYCLING SERVICES PH: (760) 340-2113

**GAS PROVIDER**  
 SOCAL GAS PH: (805) 423-1351

**ELECTRICITY PROVIDER**  
 SOUTHERN CALIFORNIA EDISON PH: (805) 255-7878

**BUSINESS OPERATION:**  
 INTENDED HOURS OF OPERATION 7AM TO 10PM FOR STOREFRONT RETAIL. NO DELIVERY SERVICE WILL BE PROVIDED FROM STORE. THE BUSINESS WILL FOCUS ON STOREFRONT RETAIL. PERISHABLE DELIVERY USAGE WILL BE LOGGED ON AT A FUTURE DATE. THE RETAIL WILL BE STAFFED WITH 20 INDIVIDUALS TOTAL (1 SALESPEOPLE, 1 MANAGER, 1 INVENTORY STOCKER) AND IT WILL BE ADJUSTED BASED ON PRODUCT DEMAND. THIS FACILITY WILL CARRY AN ADULT AND MEDICAL LICENSE.

**GENERAL NOTES:**

- PURSUANT TO SECTION 18.08 P. ALL COMMERCIAL CANNABIS ACTIVITIES SHALL BE SITED AND OPERATED IN A MANNER THAT PREVENTS CANNABIS HARBORAGE FROM BEING IDENTIFIED OFFSITE IN ORDER TO CONTROL HARBORAGES SUCH AS DOORS, HUMIDITY AND WELD. COMMERCIAL CANNABIS ACTIVITIES SHALL INSTALL AND MAINTAIN AT THE MINIMUM, THE FOLLOWING EQUIPMENT, OR ANY OTHER EQUIPMENT THAT CAN BE PROVEN TO BE AN EQUALLY OR MORE EFFECTIVE METHOD OR TECHNOLOGY TO CONTROL THESE HARBORAGES:
  - AN EXHAUST AIR EXHAUST SYSTEM WITH DOOR CONTROL THAT PREVENTS INTERNAL CANNABIS FROM BEING SMITTED EXTERNALLY.
  - AN AIR SYSTEM THAT CONTROLS RELATIVE AIR PRESSURE BETWEEN THE COMMERCIAL CANNABIS ACTIVITIES INTERIOR AND EXTERIOR.
- PURSUANT TO SECTION 18.08 P. ALL COMMERCIAL CANNABIS ACTIVITIES THAT UTILIZE HAZARDOUS MATERIALS SHALL COMPLY WITH APPLICABLE HAZARDOUS WASTE GENERATOR, HAZARDOUS WASTE OR HAZARDOUS MATERIALS HANDLING, STORAGE, TRANSPORTATION, AND DISPOSAL REGULATIONS AND MAINTAIN ANY APPLICABLE PERMITS FOR THESE PROGRAMS FROM THE RIVERSIDE COUNTY FIRE DEPARTMENT, THE RIVERSIDE COUNTY DEPARTMENT OF ENVIRONMENTAL HEALTH, THE RIVERSIDE COUNTY DEPARTMENT OF WASTE RESOURCES AND THE AGRICULTURAL COMMISSIONER.
- NO LOTTERY LICENSE TO BE ACQUIRED BY BUREAU DEPARTMENT.

**SHEET INDEX**

SHEET ISSUED ON DATE INDICATED, WITH MODIFICATIONS ●  
 SHEET ISSUED ON DATE INDICATED, NO MODIFICATIONS ○

NO.	TITLE & SHEET	DATE	BY	CHKD	APP'D
AK.1	TITLE SHEET				
AL.0	EXISTING SITE PLAN				
AL.1	PROPOSED SITE PLAN				
AL.2	EXISTING FLOOR PLAN				
AL.3	PROPOSED FLOOR PLAN				
AL.4	SECURITY PLAN				
AL.5	POSTING ELEVATION ELEVATIONS				
AL.6	EXISTING ELEVATION ELEVATIONS				
AL.7	PROPOSED ELEVATION ELEVATIONS				
AL.8	POSTING CONDITIONS & ADJACENT SITES				
AL.9	EXTERIOR BUILDING SIGNAGE				
AL.10	INDOOR SIGNAGE				
AL.11	TEMPERARY LANDSCAPE PLAN				
AL.12	PRELIMINARY IRIGATION & DRAINAGE PLAN				
AL.13	EXISTING SITE PHOTO				

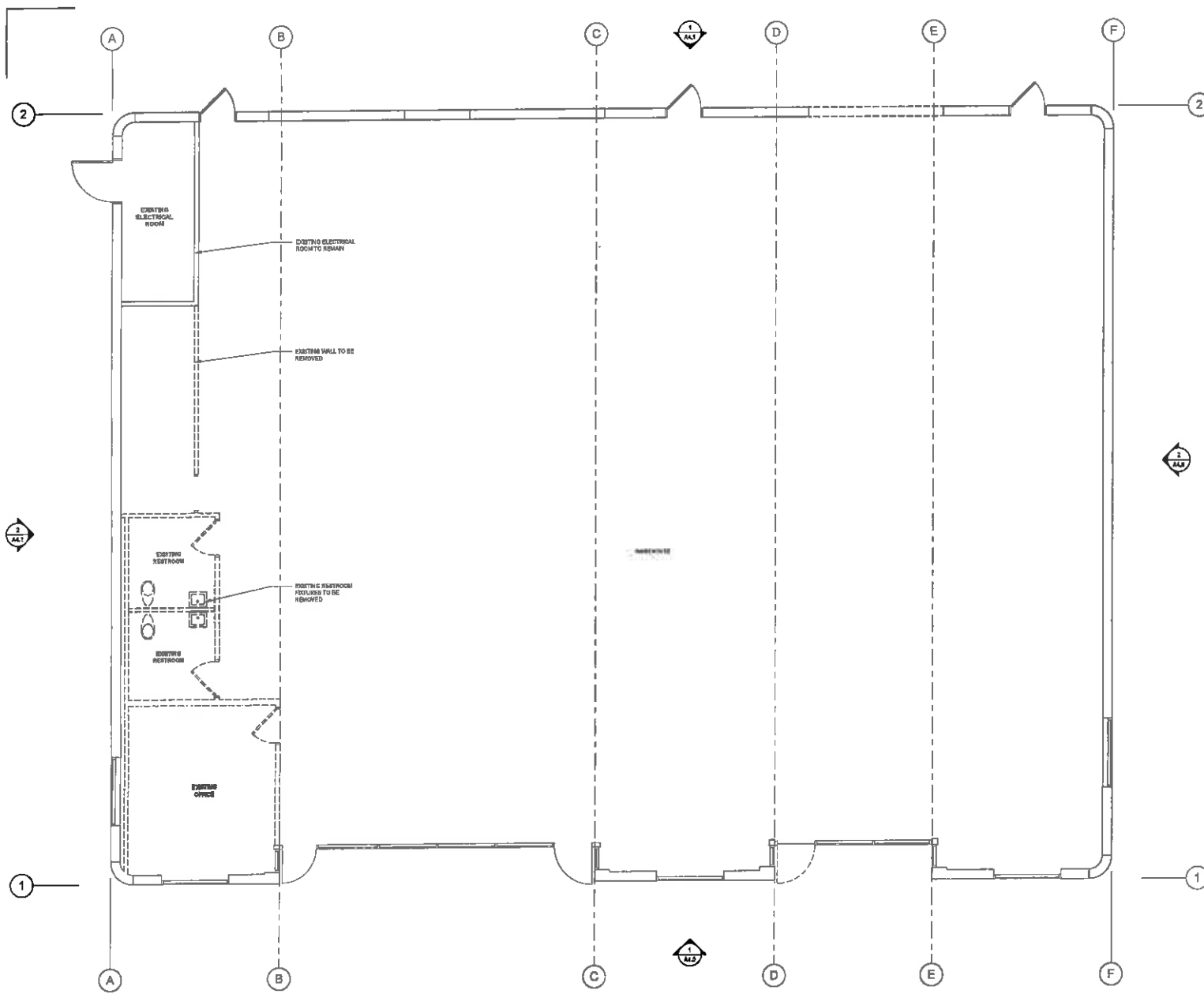
**GENERAL PROJECT DATA**

**BUILDING DEPARTMENT:** \_\_\_\_\_ COUNTY OF RIVERSIDE  
**ZONE:** \_\_\_\_\_ LP INDUSTRIAL PARK  
**LAND USE DESIGNATION:** \_\_\_\_\_ U (LIGHT INDUSTRIAL)  
**ASSESSOR'S PARCEL NO.:** \_\_\_\_\_ 034-23-028  
**OCCUPANCY CLASSIFICATION:** \_\_\_\_\_ PER CBC CHAPTER 1 M, F-1 AND S-1  
**OCCUPANCY SEPARATION:** \_\_\_\_\_ PER CBC TABLE 804.4 - NO SEPARATION REQUIRED  
**TYPE OF CONSTRUCTION:** \_\_\_\_\_ PER CBC CHAPTER 9 (SECTION 902) - VS (EXISTING)  
**SPRINKLERED:** \_\_\_\_\_ YES  
**NUMBER OF STORIES:** \_\_\_\_\_ 1 STORY  
**PROJECT SCOPE:** \_\_\_\_\_ SCOPE OF WORK ENCLOSED IS 2000 SQ. FT. OF TENANT IMPROVEMENTS OVER AN 8,123 SQ. FT. LAND.  
**APPLICANT**  
 DKJ GROUP INC.  
 JREVELOPER@DKJGROUP.COM JOE LIZARRADA  
 2024 EAST CHAPMAN AVE, 1510 PH: (951) 250-6073  
 ORANGE, CA 92668  
**ARCHITECT**  
 INFRASTRUCTURE ENGINEERS  
 801 BRAZOS INFRASTRUCTUREARCHITECTS.COM  
 222 W. HARBOUR BLVD., SUITE 706 ANAHEIM, CA 92805  
 SIA: 01/19/23 P: (951) 122-8777 F: (714) 840-8700  
**LANDSCAPE ARCHITECT**  
 COLLEEN M. HO, JAN COLLEEN M. HO, JAN  
 5265 SILVERADO COURT CORDONA, CA 92883 PH: (714) 743-7918  
**VICINITY MAP**



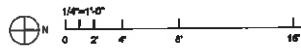






**WALL LEGEND**

- EXISTING WALL TO REMAIN
- EXISTING WALL TO BE REMOVED
- EXISTING WALL TO BE DISCOLOR/ED



**EXISTING FLOOR PLAN**

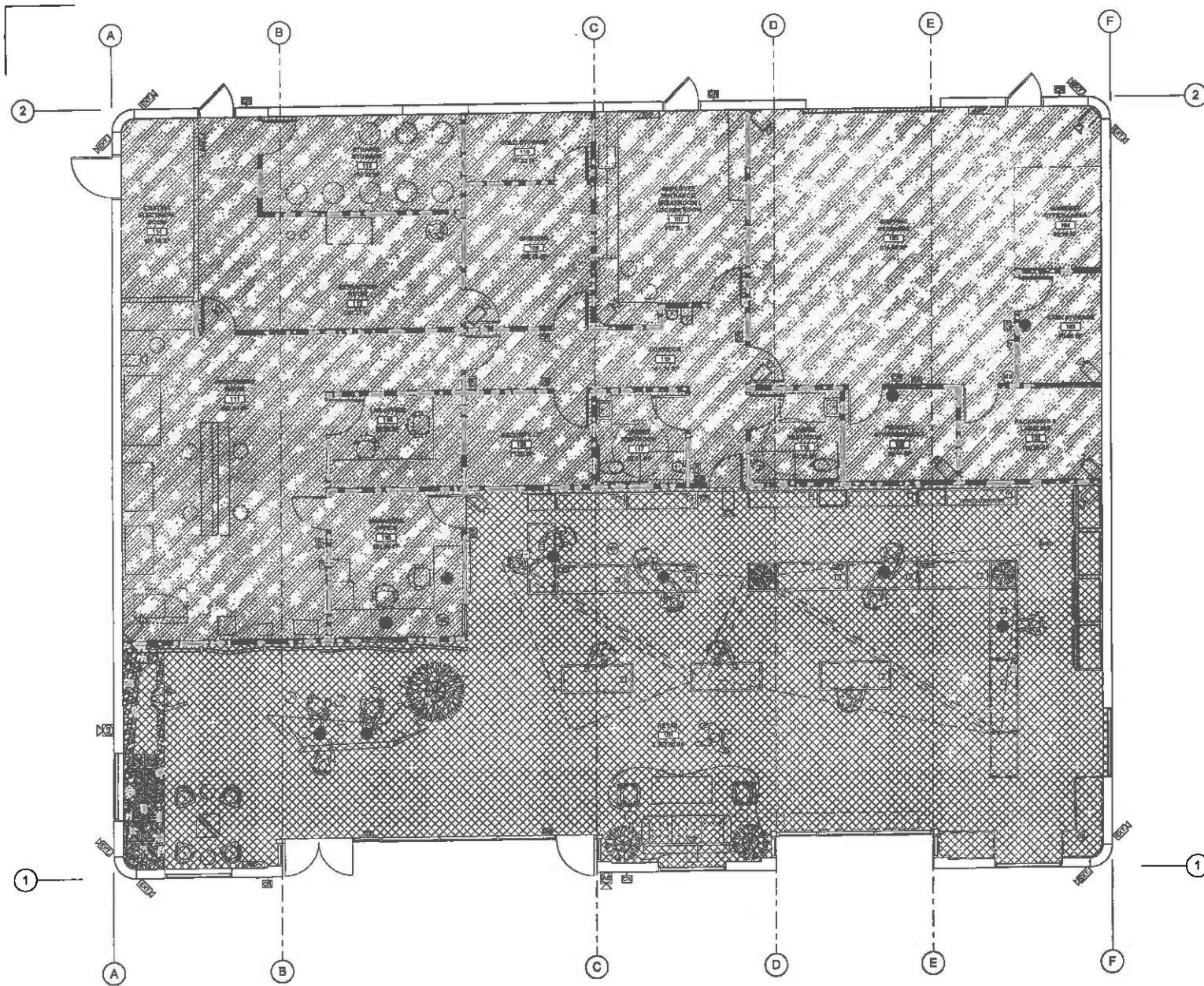
DIAMOND SAP  
77704 FLORA RD.  
PALM DESERT, CA 92211

CONDITIONAL USE PERMIT

123456789 ENGINEERS 123456789 123456789 123456789	PROJECT NUMBER: 2041.01 DATE: 12/20/2020 CAP_SUBMITTAL_1

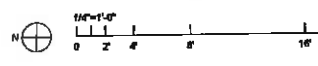
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**LEGEND**

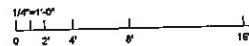
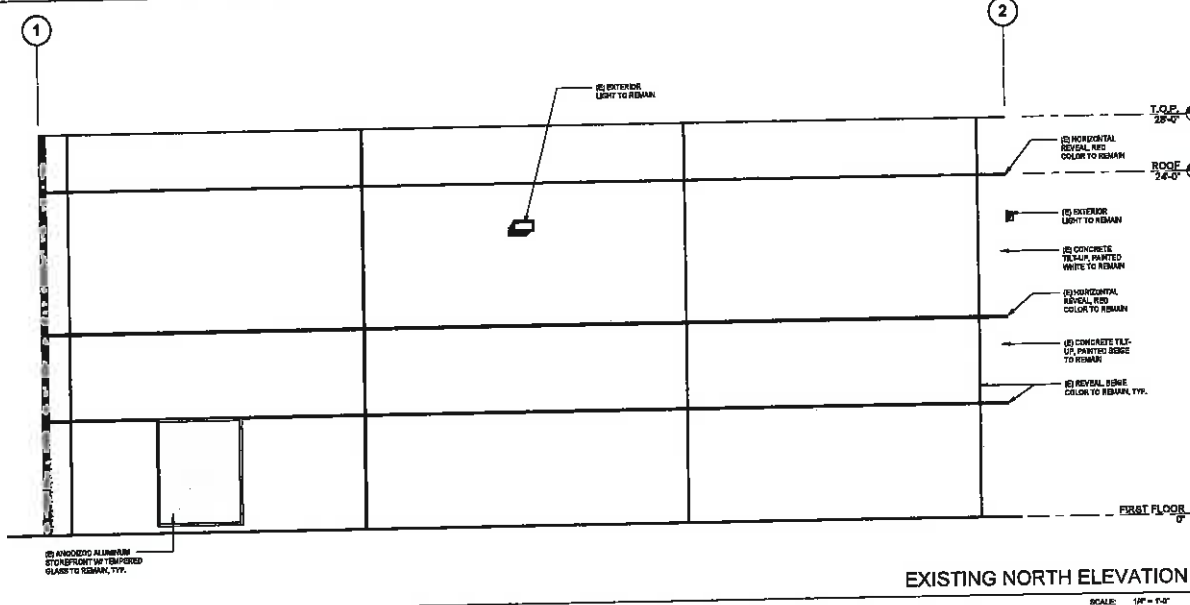
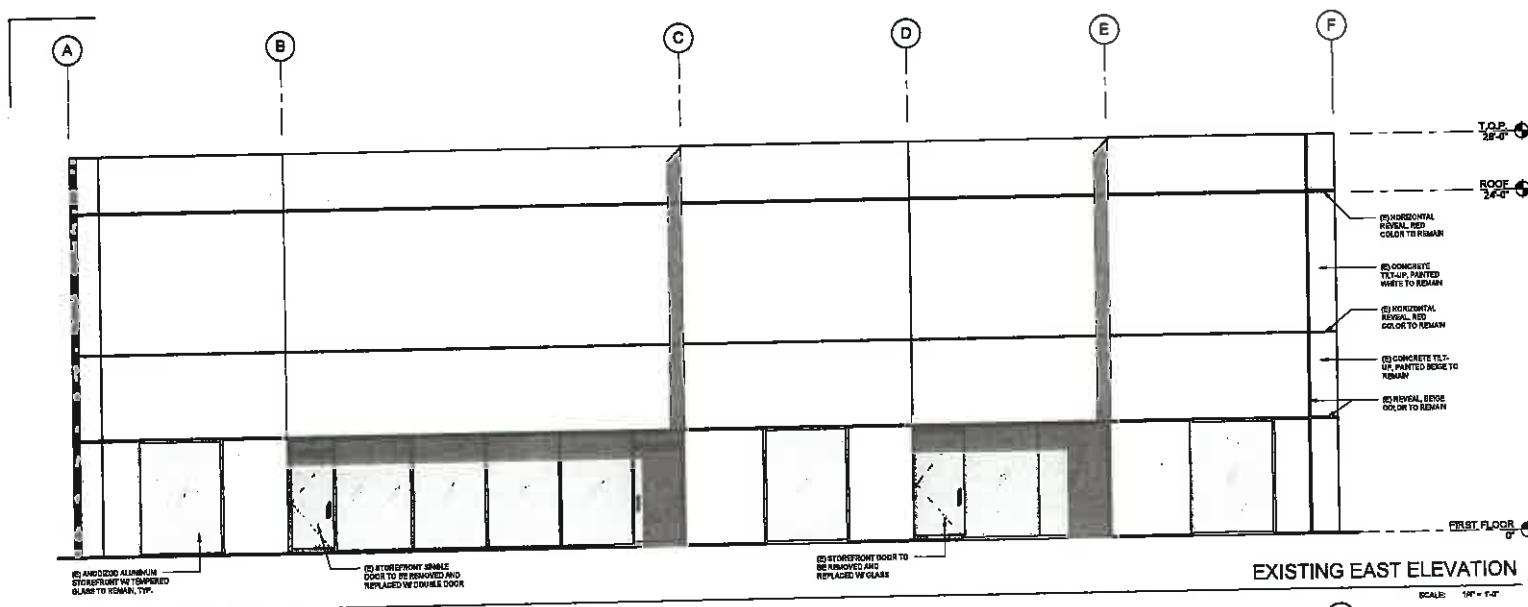
- INTERIOR CAMERA
- EXTERIOR CAMERA
- MOTION SENSOR
- SMOKE SENSOR
- ALARM SENSOR
- CIRCUIT BREAKER
- LIMITED ACCESS
- RESTRICTED ACCESS
- PUSH BUTTON
- ALARM BELL



CONDITIONAL USE PERMIT

**SECURITY PLAN**  
 DIAMOND S&P  
 7704 FLORA RD.  
 PALM DESERT, CA 92211

	PROJECT NUMBER: 2044.01 DATE: 08/08/2008 CUP_RESUBMITTAL_1	SHEET <b>A3.0</b>
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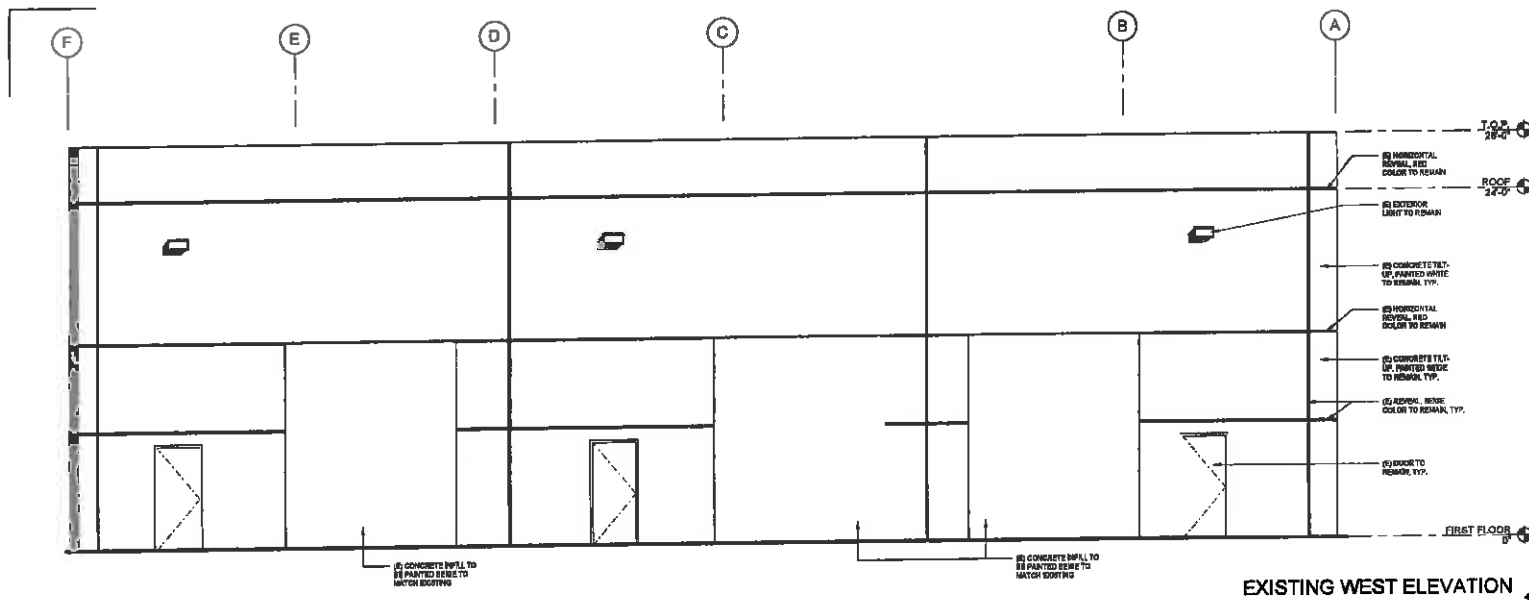


EXISTING EXTERIOR ELEVATIONS

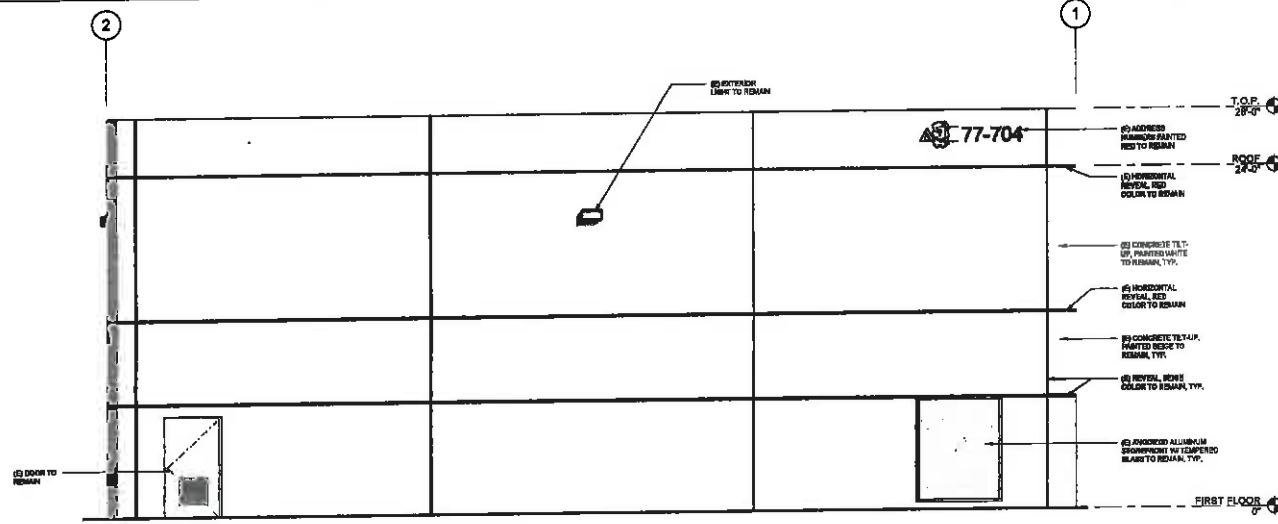
CONDITIONAL USE PERMIT

DIAMOND SAP  
77704 FLORA RD.  
PALM DESERT, CA 92211

	PROJECT NO. 2402	DIST.
	DATE SUBMITTED: 01/14/2014	AA.0



**EXISTING WEST ELEVATION**  
SCALE: 1/4" = 1'-0"



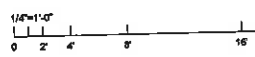
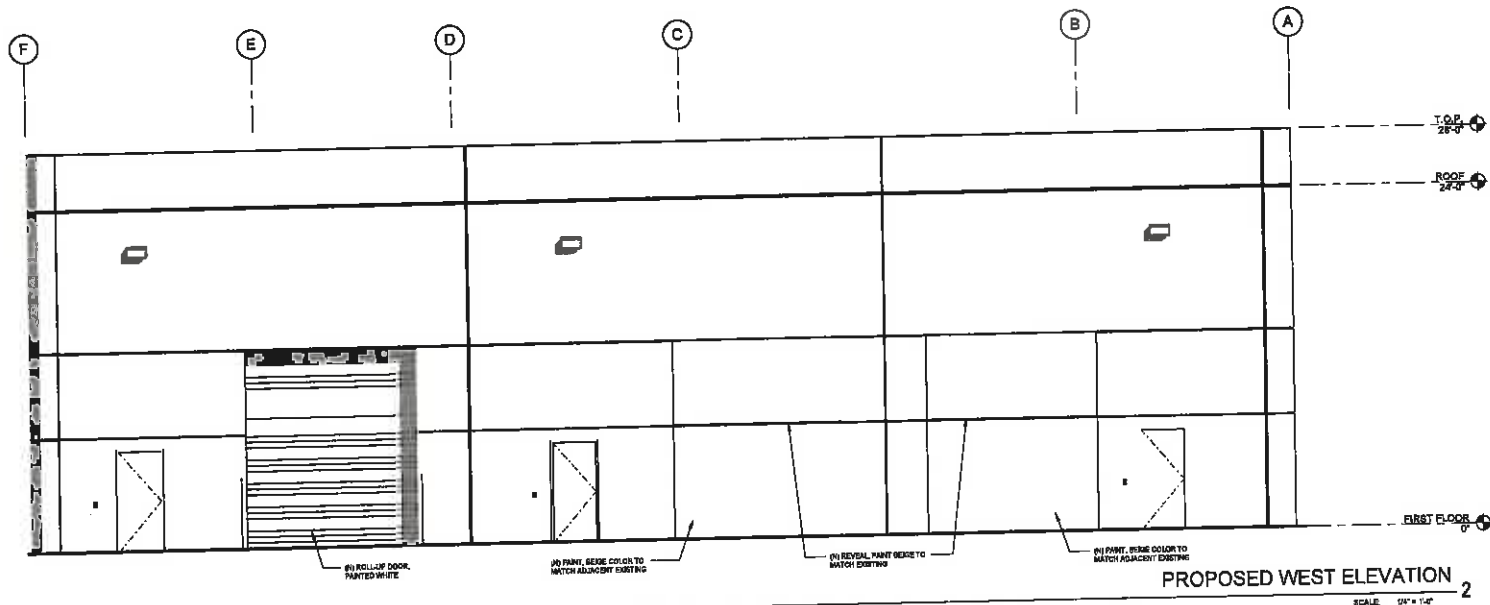
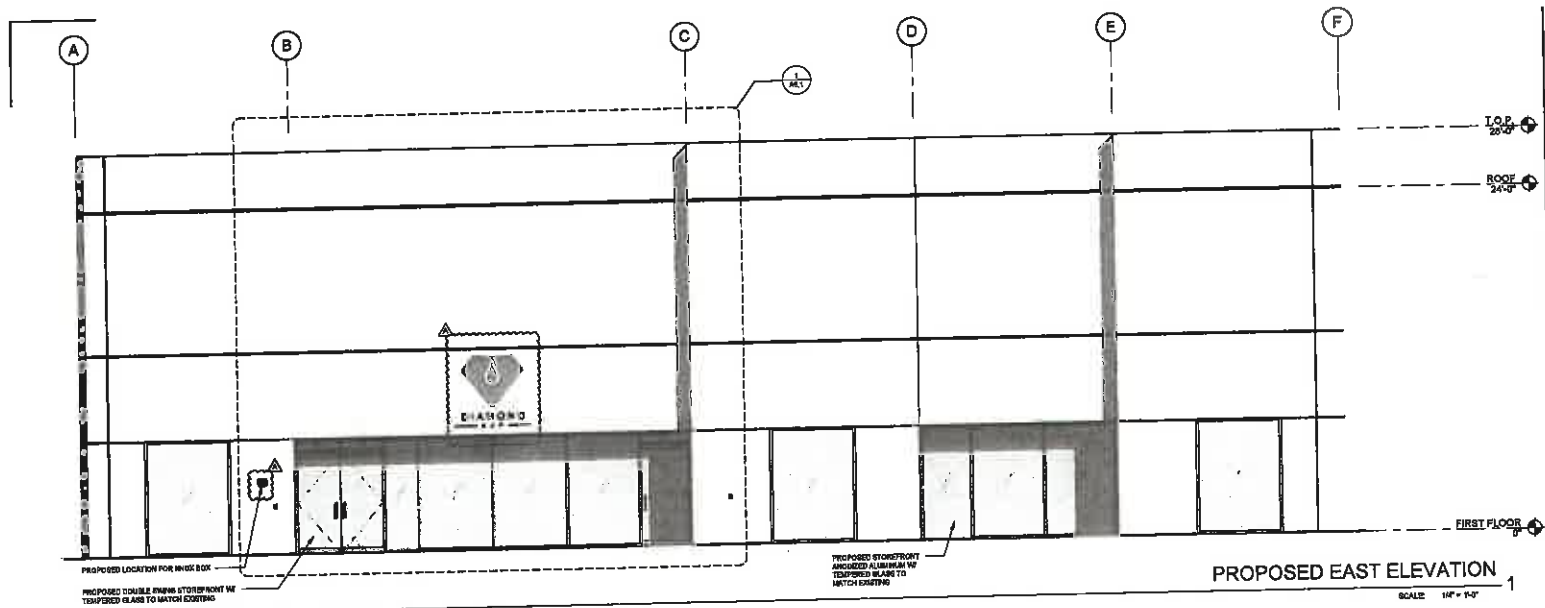
**EXISTING SOUTH ELEVATION**  
SCALE: 1/4" = 1'-0"



**EXISTING EXTERIOR ELEVATIONS**  
DIAMOND BAP  
77704 FLORA RD.  
PALM DESERT, CA 92211  
CONDITIONAL USE PERMIT



PROJECT NUMBER: 2014.01  
DATE: 02/08/2015  
SHEET: A4.1



PROPOSED EXTERIOR ELEVATIONS  
 DIAMOND S&P  
 77704 FLORA RD.  
 PALM DESERT, CA 92211

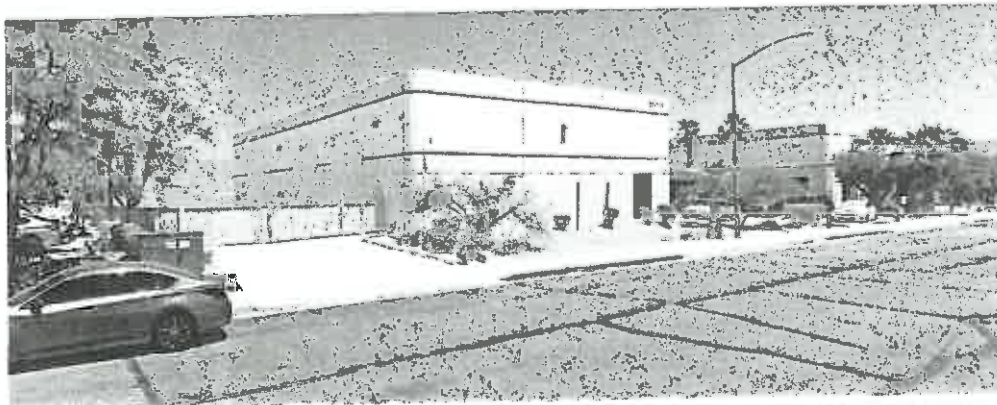
REVISIONS	
NO.	DESCRIPTION

PROJECT NUMBER: 2044.01  
 DATE: 02/02/2010 CLIP\_RESUBMITTAL\_1  
 SHEET: A4.2





- 1 -



- 2 -

**EXISTING CONDITIONS & ADJACENT SITES**

CONDITIONAL USE PERMIT

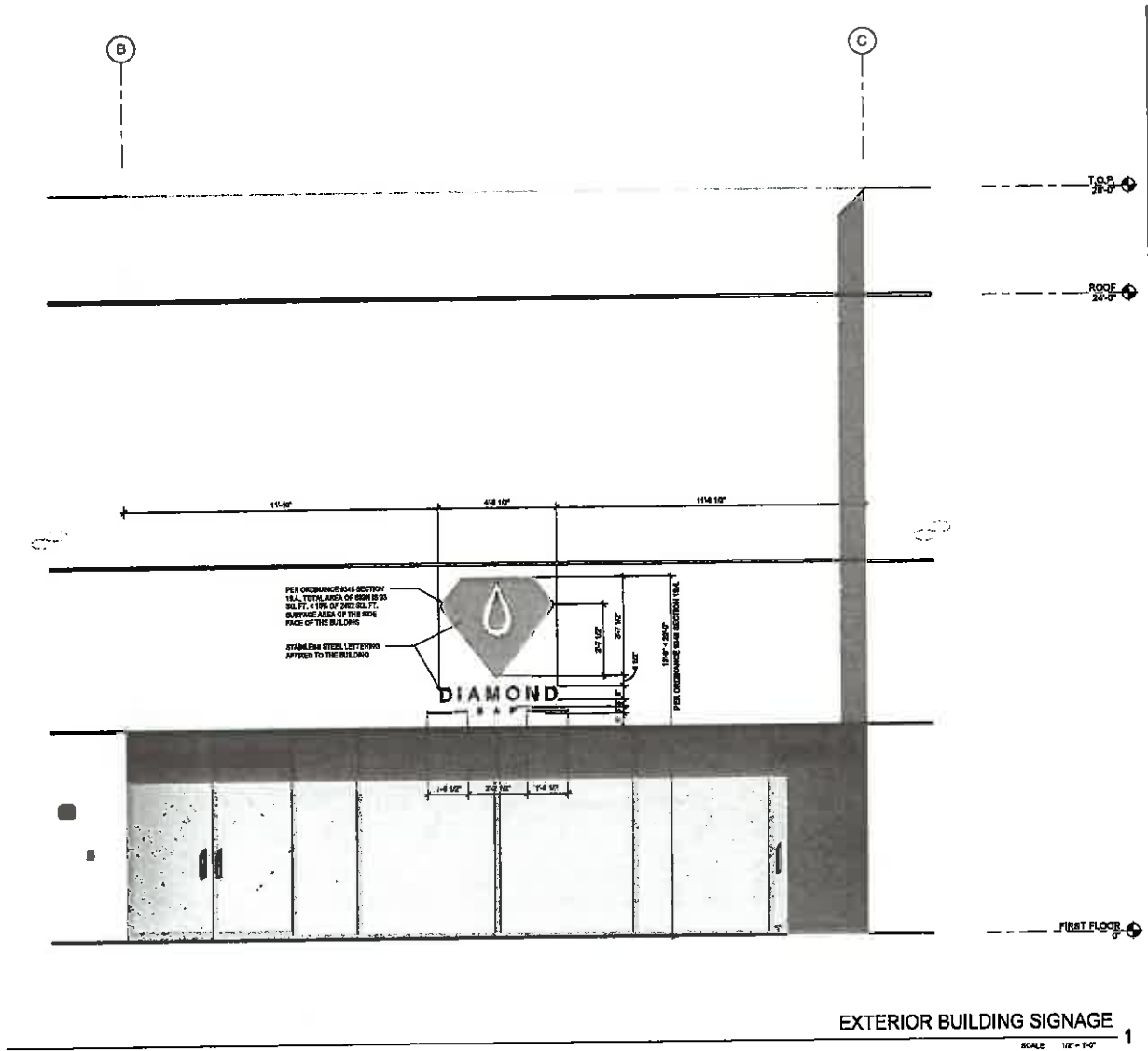
**DIAMOND RAP**  
77704 FLORA RD.  
PALM DESERT, CA 92211



2014 Professional Seal No. 100  
State of California  
Civil Engineer

PROJECT NUMBER: 2014-01  
DATE: 2014-08-01 (CLIP\_REVISIONS)\_1

SHEET  
A5.0



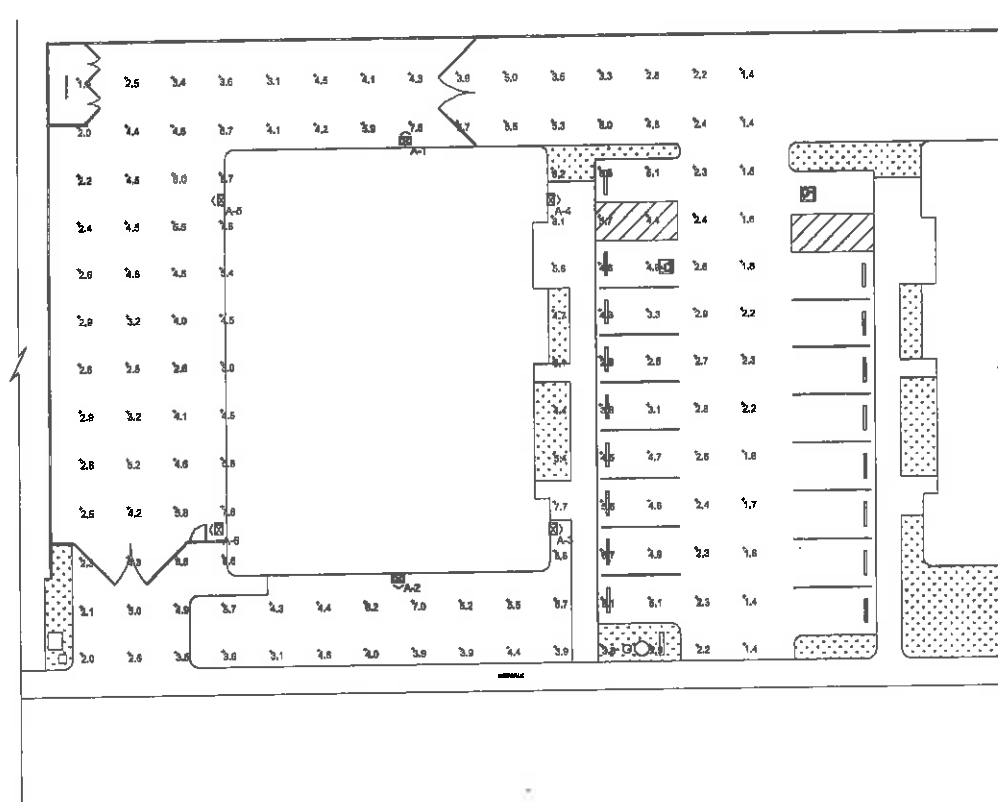
CONDITIONAL USE PERMIT

**EXTERIOR BUILDING SIGNAGE**

DIAMOND S&P  
77704 FLORA RD.  
PALM DESERT, CA 92211

	PROJECT NUMBER: 202101	SHEET
	DATE: 08/08/2021 04:58:00 PM	AS 1





Plan View  
Scale - 1" = 10'

Schedule						
Symbol	Label	QTY	Category Number	Description	Lamp	Notes
	A	6	2834-C-70-60-2-3K @ 20 WALL MOUNT	2834-C-70-60-2-3K W x 6 in. H LED LINNAIRE		

Surfaces						
Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
A	+	4.1 E	8.8 W	1.4 E	8.21	2.87

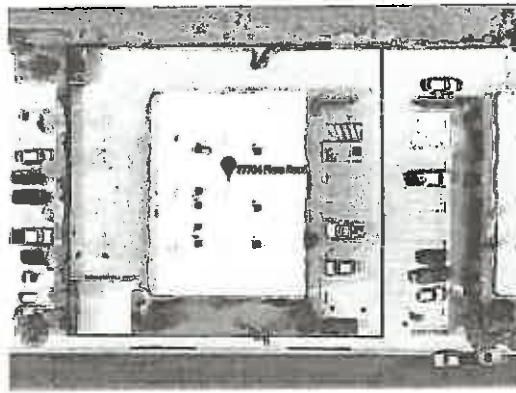
DIAMOND SAP  
77704 FLORA RD.  
PALM DESERT, CA 92211



PROJECT NUMBER: 2041.01  
DATE: 10/24/2018 - CUP  
PHOTOMETRIC STUDY  
SHEET E-1.0





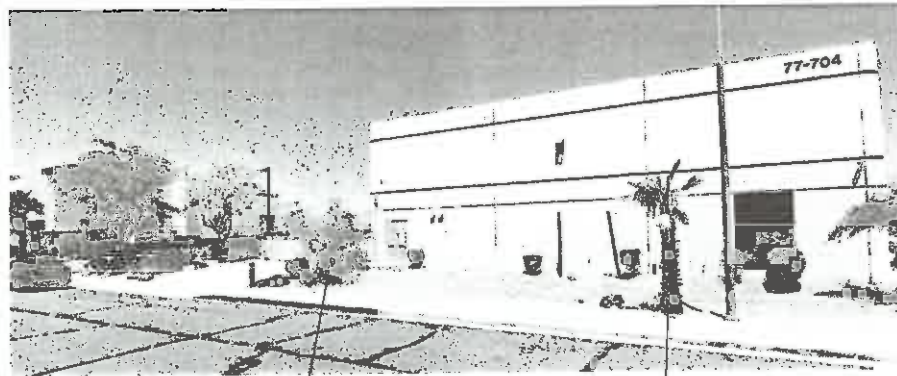


EXISTING PROJECT SITE



EXISTING PROJECT SITE

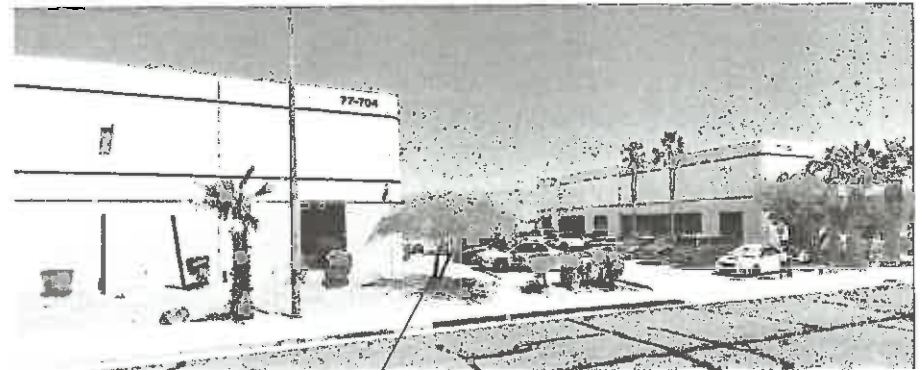
ADJACENT BUSINESS PROPERTIES



STREET VIEW OF PROJECT SITE

EXISTING TREE TO REMAIN, PROTECT IN PLACE

EXISTING PALM TREE TO REMAIN, PROTECT IN PLACE



STREET VIEW OF PROJECT SITE

EXISTING TREE TO REMAIN, PROTECT IN PLACE



THE PROPOSED PLANTING SHALL MIMIC THE EXISTING LANDSCAPE AT THE ADJACENT PROPERTIES

PROJECT APPLICATOR  
 DEJ DESIGN INC  
 3000 WILLOW HOLLOW BLVD  
 JOE IDARABARA  
 951.265.6370

CONDITIONAL USE PERMIT

EXISTING SITE PHOTOS  
 DIAMOND SAP  
 77-704 FLORA RD.  
 PALM DESERT, CA 92211

DATE	REMARKS
10/24/2016	CUP
11/02/2016	CUP RESUBMITTAL 1



Colleen M. Nolan  
 Landscape Architect 15999  
 cnolan@nolan.net  
 714.743.7915 cell

APN#: 624-420-025



PROJECT NUMBER: 20144.01  
 DATE: 02/08/2020 - CUP  
 RESUBMITTAL 1

SHEET  
 L1.3

**PAGE BREAK**



# AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY



June 18, 2020

Mr. Matthew Taylor, Senior Planner  
City of Riverside Community Development Department Planning Division  
3900 Main Street, 3<sup>rd</sup> Floor  
Riverside CA 92522

**CHAIR**  
Russell Betts  
Desert Hot Springs

**VICE CHAIR**  
Steven Stewart  
Palm Springs

**COMMISSIONERS**

Arthur Butler  
Riverside

John Lyon  
Riverside

Steve Manos  
Lake Elsinore

Richard Stewart  
Moreno Valley

Gary Youmans  
Temecula

**STAFF**

Director  
Simon A. Housman

John Guerin  
Paul Rull  
Barbara Santos

County Administrative Center  
4080 Lemon St., 14<sup>th</sup> Floor.  
Riverside, CA 92501  
(951) 955-5132

[www.rcaluc.org](http://www.rcaluc.org)

## **RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW – DIRECTOR’S DETERMINATION**

File No.: ZAP1052RG20  
Related File No.: P20-0179 (Zoning Ordinance Amendment), P20-0190 (Specific Plan Amendment), P20-0191 (Specific Plan Amendment)  
APN: Citywide

Dear Mr. Taylor:

As authorized by the Riverside County Airport Land Use Commission (ALUC) pursuant to its Resolution No. 2011-02, as ALUC Director, I have reviewed City of Riverside Case Nos. P20-0179 (Zoning Ordinance Amendment), P20-0190 (Specific Plan Amendment), and P20-0191 (Specific Plan Amendment), all of which are City-initiated legislative amendments. P20-0179 is a proposal to amend the City’s Zoning Code (Title 19) implementing the policies of the revised Good Neighbor Guidelines for Siting New and/or Modified Industrial Facilities. Specifically, the amendment will incorporate these policies in the following sections: Base Zones and Related Use Development Provisions, Specific Land Use Provisions, Site Planning and General Development Provisions, and Definitions. The amendment includes the addition of a new chapter (19.435) providing updated development criteria for warehousing and distribution facilities.

The proposed amendment limits the maximum gross floor area, excluding mezzanines, of buildings in the BMP, I, and AIR industrial zones within 200 feet of residential zones or uses to 10,000 square feet and limits building height in those areas to 35 feet. The maximum gross floor area, excluding mezzanines, of buildings in the BMP, I, and AIR industrial zones that are at least 200 feet from, but within 800 feet of, residential zones or uses is limited to 100,000 square feet. Side and rear yard setbacks adjacent to residential zones or uses are increased to 60 feet. New development or substantial enlargement of industrial uses within 1,000 feet of a residential zone or use will require preparation of a Health Risk Assessment prepared in accordance with South Coast Air Quality Management District guidelines.

P20-0190 is a proposal to amend the Hunter Business Park Specific Plan to incorporate the policies of the Good Neighbor Guidelines for Siting New and/or Modified Industrial Facilities, while P20-0191 is a proposal to similarly amend the Sycamore Canyon Business Park Specific Plan. These Specific Plans are located within the March Air Reserve Base/Inland Port Airport Influence Area. The proposed amendments include revisions to the Industrial Zones chapter modifying building setback and height requirements and establishing maximum building size within certain proximities to residential land uses, revisions to Base Zones Permitted Land Uses Permitted Use Table to modify required entitlements, addition of a new chapter establishing

## AIRPORT LAND USE COMMISSION

specific use regulations including site location, development, and operational standards, modifications to outdoor storage and truck yard screening, addition of warehouse definitions, amending the following sections: Development Standards and Criteria, Development Standards and Design Guidelines, and other minor non-substantive changes and technical corrections.

There are no additions to the permitted land use tables and no development standard changes that would increase residential density or non-residential intensity proposed through these amendments. Therefore, these amendments have no possibility for having an impact on the safety of air navigation within airport influence areas located within the City of Riverside.

As ALUC Director, I hereby find the above-referenced projects **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port, 2005 Riverside Municipal, and 2004 Flabob Airport Land Use Compatibility Plans.

If you have any questions, please contact Paul Rull, ALUC Principal Planner, at (951) 955-6893.

Sincerely,

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



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Simon A. Housman, ALUC Director

Attachments for Airport Managers: Proposed Zoning Ordinance Amendment  
Proposed Specific Plan Amendments

cc: Kim Ellis, Manager, Riverside Municipal Airport  
Gary Gosliga, March Inland Port Airport Authority  
Doug Waters, Deputy Base Civil Engineer, March Air Reserve Base  
Beth LaRock, Flabob Airport

Y:\AIRPORT CASE FILES\Regional\ZAP1052RG20\ZAP1052RG20.LTR.doc

**P20-0179**

**Zoning Code Amendment**



## Chapter 19.130 - INDUSTRIAL ZONES (BMP, I, AI AND AIR)

### 19.130.010 - Purpose.

The industrial zones are established to implement the Business/Office Park and Industrial land use categories of the General Plan. The purpose of the industrial zones is to provide areas appropriate for a wide variety of industrial, manufacturing, and support uses that have the potential to provide jobs and generate tax revenue in Riverside, while protecting residential neighborhoods, schools, parks, playgrounds, day care centers, nursing homes, hospitals and other public places from nuisances or hazards associated with such activities. Commercial uses allowed in the industrial zones are intended to serve the needs of the industrial and surrounding uses and their employees, and generally are not intended to draw customers from a larger region. The purpose of each of the industrial zones is as follows:

- A. Business and Manufacturing Park Zone (BMP). The Business and Manufacturing Park Zone (BMP) is established to provide a district for low-intensity and low-impact industrial, office, and related uses. Typical uses include research and development facilities and laboratories; administrative, executive and professional offices; small-scale warehouses, light manufacturing; and support commercial.
- B. General Industrial Zone (I). The General Industrial Zone (I) is established to provide a district for general manufacturing and wholesaling, limited warehousing and distribution facilities, and support commercial.
- C. Air Industrial Zone (AI). The Air Industrial Zone (AI) is established to help protect the health, safety and general welfare of the Riverside Municipal Airport and its environs, including provisions for sensible growth and high-quality development tailored to the varied development conditions of the Airport area. The AI Zone consists of four subzones - AI-1, AI-2, AI-3, and AI-4 - with the following primary purposes:
  1. AI-1: To establish areas for businesses needing large-scale, high-visibility locations.
  2. AI-2: To establish areas for airport industrial, office and limited commercial uses needing moderate scale sites at or adjacent to the Riverside Municipal Airport.
  3. AI-3: To establish areas for moderate-size uses with varying locations and site size needs.
  4. AI-4: To establish areas for small-size uses with limited visibility and site size needs.
- D. Airport Zone (AIR). The Airport Zone (AIR) is established in recognition of the importance of airport and aviation-related uses to the City's economy, and to allow for aviation, industrial, service and commercial uses related to or compatible with or convenient for airport operations.

(Ord. 7331 §7, 2016; Ord. 6966 §1, 2007)

### 19.130.020 - Permitted land uses.

- A. Table 19.150.020.A (Permitted Uses Table), Table 19.150.020.B (Incidental Uses Table and Table 19.150.020.C (Temporary Uses Table) in Chapter 19.150 (Base Zones Permitted land uses) identify permitted uses, permitted accessory uses, permitted temporary uses, and uses permitted subject to the approval of a minor conditional use permit (Chapter 19.730 - Minor Conditional Use Permit), or conditional use permit (Chapter 19.760 - Conditional Use Permit), or uses requiring some other permit. Table 19.150.020.A also identifies those uses that are specifically prohibited. Uses not listed in the tables or in Section 19.130.025 (Uses Specifically Prohibited) are prohibited unless, the Community & Economic Development Director or his/her designee, pursuant to Chapter 19.060 (Interpretation of Code), determines that the use is similar to and no more detrimental than a listed permitted or conditional use. Any use which is prohibited by state and/or federal law is also strictly prohibited.

- B. Chapter 19.149 - Airport Land Use Compatibility includes Airport Land Use Compatibility Plan requirements for discretionary actions proposed on property located within an Airport Compatibility Zone.

(Ord. 7413, § 1(Exh. A), 2-20-2018; Ord. 7331 §7, 2016; Ord. 7064 §4, 2010; Ord. 6966 §1, 2007)

**19.130.025 - Uses specifically prohibited.**

- A. The following uses are specifically prohibited in any industrial zone:

1. Any residential use other than permitted caretaker housing
2. Ammonia, chlorine or bleaching powder manufacture
3. Animal slaughtering
4. Asphalt batching plant and rock, sand and gravel pit crushing and screening plant
5. Automobile wrecking, dismantling, and salvage yards
6. Bone, coal or wood distillation
7. Brick, tile or clay products manufacture
8. Cement, lime, gypsum or plaster of Paris manufacture, except that the manufacture of cement products shall be permitted
9. Explosive manufacture or storage
10. Fat rendering
11. Fertilizer manufacture
12. Foundry or smelting of ferrous metals, steel mill or boiler works
13. Garbage, offal, dead animal or refuse incineration, reduction or dumping
14. Glue manufacture
15. Hydrochloric, nitric or sulfuric acid manufacture
16. Junk, rag or scrap iron storage yards or bailing
17. Petroleum refining or petroleum products manufacture
18. Rock, sand or gravel excavating
19. Rubber or gutta-percha manufacture
20. Soap manufacture
21. Stockyard, cattle-feeding yard or hog ranch
22. Tallow, grease or lard manufacture or refining
23. Tanning, curing or storing of raw hides or skins
24. Other uses that, by written decision of the Community & Economic Development Director or his/her designee, are determined to be detrimental to the public welfare by reason of the emission of odor, dust, smoke, gas, noise, vibration or other causes.

(Ord. 7331 §7, 2016; Ord. 6966 §1, 2007)

**19.130.030 - Development standards for Industrial Zones.**

Table 19.130.030.A (BMP, I and AIR Industrial Zones Development Standards) sets forth the minimum development standards for all development in the BMP, I, and AIR Zones. Table 19.130.030.B (AI Industrial Zones Development Standards) sets forth the minimum development standards for all development in the AI Zones.

(Ord. 7331 §7, 2016; Ord. 6966 §1, 2007)

**Table 19.130.030.A  
BMP, I and AIR Industrial Zones Development Standards**

Development Standards	BMP	I	AIR	Notes, Exceptions & Special Provisions
Floor-Area-Ratio (FAR) - Maximum <sup>1, 3</sup>	1.50	0.60	0.60	See Chapter 19.149-Airport Land Use Compatibility
Lot Area - Minimum	40,000 sq. ft. <sup>2</sup>	10,000 sq. ft.	8,000 sq. ft.	
Lot Width - Minimum	140 ft.	60 ft.	60 ft.	
Lot Depth - Minimum	100 ft.	100 ft.	100 ft.	
Building Height - Maximum <sup>3</sup>	45 ft.	45 ft.	45 ft.	See Chapter 19.149-Airport Land Use Compatibility
a. Within 200 feet of a Residential Zone or use	35 ft.	35 ft.	35 ft.	
b. All other locations	45 ft.	45 ft.	45 ft.	
Building Size - Maximum				Gross floor area, exclusive of mezzanine. Multiple buildings allowed provided the maximum FAR is not exceeded.
a. Within 200 feet of a Residential Zone or use	10,000 sq. ft.	10,000 sq. ft.	10,000 sq. ft.	
b. 200-800 feet of a Residential Zone or use	100,000 sq. ft.	100,000 sq. ft.	100,000 sq. ft.	
c. All other locations	Per FAR	Per FAR	Per FAR	
Front Yard Setback - Minimum <sup>5, 6</sup>	—	20 ft.	15 ft.	In the BMP Zone, 20-feet of the required 50-foot front yard setback shall be landscaped. However, a 40-foot front yard setback shall be permitted if it is landscaped in its entirety.
a. Buildings over 30 ft. in height or on an arterial street	50 or 40 ft. <sup>4</sup> (See Notes)	—	—	
b. Buildings 30 ft. or less in height and not on an arterial street	20 ft. (See Notes)	—	—	In the BMP zone, the 20-foot front yard setback required for buildings 30-feet or less in

				height shall be landscaped in its entirety.
Side Yard setbacks - Minimum <sup>85</sup>				
a. Interior Side-side	0 ft.	0 ft.	0 ft.	
b. Adjacent to Residential Zone or use	60	60	60	Not less than 15 feet of the minimum side yard setback area directly adjacent to a Residential Zone or use shall be fully landscaped.
b-c.b. Adjacent to Street or Alley Street side	Same as Front Yard	20 ft. <sup>7</sup>	15 ft. <sup>7</sup>	
Rear Yard Setback - Minimum <sup>9</sup>	0 ft.	0 ft.	15 ft.	
a. Rear yard	0 ft.	0 ft.	15 ft.	
b. Adjacent to Residential Zone or use	60 ft.	60 ft.	60 ft.	Not less than 15 feet of the minimum rear yard setback area directly adjacent to a Residential Zone or use shall be fully landscaped.
a-c. Adjacent to Streets	Same as Front Yard	20 ft. <sup>7</sup>	20 ft. <sup>7</sup>	

Notes:

1. The Approving or Appeal Authority may allow a development project to exceed a maximum FAR when findings can be made that such project (a) will not have a detrimental effect on infrastructure and municipal services, (b) will not adversely impact the surrounding neighborhood, and (c) will not likely set a precedent for additional development that would adversely affect infrastructure, service or surrounding land uses.
2. Smaller minimum lot areas may be established by a specific plan or master plan in the BMP Zone. A master plan must include provisions for common access, parking and maintenance. A total master plan area of five acres is required. Site plan review approval by the Community & Economic Development Director or his/her designee is required for any master plan.
3. See Chapter 19.149 - Airport Land Use Compatibility to determine if a project site is subject to Airport Land Use Compatibility Plan requirements.
4. In the BMP Zone, off-street parking, gate or guard houses, roofs or canopies covering unenclosed pedestrian walks and walls or fences not more than six feet in height shall be permitted in the rear 30 feet of the required 50-foot front yard setback.
5. ~~Sidewalks, vehicular access drives and railroad tracks that are perpendicular to the front property line shall be permitted in the front setback of the BMP, I and AIR Zones.~~
6. ~~Meter pits, and utility manholes extending not more than six inches above the finished grade, light fixtures and any recording instruments required by this chapter shall be permitted in the front yard of any industrial zone.~~
75. A minimum ten-foot on-site landscape planter shall be required along the street side and rear yards of the I and AIR Zones.
86. A minimum side-front yard setback of 50 feet shall be required and maintained wherever a lot or parcel in any industrial zone abuts or is adjacent to a lot or parcel in any residential zone or use.
9. ~~A minimum rear yard of 50 feet shall be required and maintained wherever a lot or parcel in any industrial zone abuts a lot or parcel in any residential zone or use.~~

**Table 19.130.030.B  
AI Industrial Zones Development Standards**

Development Standards	Zones			
	AI-1	AI-2	AI-3	AI-4
Floor Area Ratio (FAR) - Maximum <sup>3, 5</sup>	1.50	1.50	1.50	1.50
Lot Area - Minimum <sup>2</sup>	5 acres			
Major Arterial Frontage		40,000 sq. ft.	40,000 sq. ft.	40,000 sq. ft.
All other streets		20,000 sq. ft.	20,000 sq. ft.	14,000 sq. ft.
Lot Width - Minimum	300 ft.			
40,000 sq. ft. Lot (Major Arterial Frontage)		140 ft.	140 ft.	140 ft.
20,000 sq. ft. Lot		100 ft.	100 ft.	
14,000 sq. ft. Lot				100 ft.
Building Height - Maximum <sup>3, 4</sup>	45 ft.	45 ft.	45 ft.	45 ft.
Front Yard Setback - Minimum	50 ft. (front 20 ft. landscaped)			
40,000 sq. ft. Lot (Major Arterial Frontage)		50 ft. (front 20 ft. landscaped)	50 ft. (front 20 ft. landscaped)	50 ft. (front 20 ft. landscaped)
20,000 sq. ft. Lot		20 ft. (all landscaped)	20 ft. (all landscaped)	
14,000 sq. ft. Lot				15 ft. (all landscaped)
Side Yard Setback - Minimum <sup>1</sup>	20 ft.			
40,000 sq. ft. Lot (Major Arterial Frontage)		20 ft.	20 ft.	20 ft.
20,000 sq. ft. Lot		None	None	

14,000 sq. ft. Lot	20 ft.			None
Rear Yard Setback - Minimum 1	20 ft.			
40,000 sq. ft. Lot (Major Arterial Frontage)		20 ft.	20 ft.	20 ft.
20,000 sq. ft. Lot		None	None	
14,000 sq. ft. Lot				None

1. **Notes:** The side or rear yard setback shall be the same as the required front yard setback wherever a side or rear yard abuts any lot zoned for residential use.
  2. Smaller minimum lot areas may be established by a specific plan or airport master plan. A master plan must include provisions for common access, parking and maintenance. A total master plan area of five acres is required. Site plan approval by the Community & Economic Development Director or his/her designee is required for any master plan.
  3. See Chapter 19.149 - Airport Land Use Compatibility to determine if a project site is subject to Airport Land Use Compatibility Plan requirements.
  4. No building, structure or tree may penetrate the flight zone of an airport per the "imaginary surfaces" established by Federal Aviation Regulations FAR Part 77.25. Any height variance will be subject to the approval of the Community & Economic Development Director or his/her designee and Airport Director.
  5. The Approving or Appeal Authority may allow a development project to exceed a maximum FAR when findings can be made that such project (a) will not have a detrimental effect on infrastructure and municipal services, (b) will not adversely impact the surrounding neighborhood, and (c) will not likely set a precedent for additional development that would adversely affect infrastructure, service or surrounding land uses.
- (Ord. 7413, § 1(Exh. A), 2-19-2018)

**19.130.040 - Additional standards, regulations and requirements for the BMP, I, AIR and AI Zones.**

- A. **Health Risk Assessment.** A Health Risk Assessment (HRA) shall be prepared in accordance with South Coast Air Quality Management District (SCAQMD) Guideline for the new development or substantial enlargement of industrial uses within 1,000 feet of a Residential Zone or use.
- B. **Walls.** Wherever a lot or parcel in any industrial zone ~~sides on or backs onto a lot or parcel in any zone that allows residential uses~~ abuts a Residential Zone or use, or ~~to abuts an alley that separates an industrial zone from any zone that allows residential uses~~ the industrial zone from a Residential Zone or use, a minimum ~~six~~ eight-foot high solid masonry wall shall be constructed along the property line or alley right-of-way line separating the industrial zone from the ~~zone that allows residential uses, or along the alley right-of-way line on the industrial side of the alley~~ Residential Zone or use. Wall height shall be measured from the finished grade of the adjacent Residential Zone or use.
  1. ~~However,~~ Such wall shall be limited in height to three feet in any required front yard or street side yard setback area.
  - 1-2. Such wall shall not be required until the industrial lot or parcel is ~~occupied~~ developed with a permitted use.

- B.C. *Outdoor display and storage.* Except for the outdoor storage and display of aircraft, outdoor display and storage shall not be permitted except as specified in 19.285 (Outdoor Storage Yard), 19.500 (Outdoor Display of Incidental Plant Materials), 19.505 (Outdoor Display and Sales - Incidental) and 19.510 (Outdoor Storage - Incidental).
- G.D. *Use of Interior Rear and Side Yards for Off-street Parking and Loading.* Except for required landscape areas, required interior rear yards and side yards may be used for off-street parking, off-street loading, outdoor storage incidental to a permitted use, and any use permitted in the required front yard area; provided such loading, parking and storage areas are acoustically shielded and screened from public view adjacent Residential Zones or uses and the public right-of-way, to the satisfaction of the Community & Economic Development Director or his/her designee.
- D.E. *Lighting.* Lighting for signs, structures, landscaping, parking areas, loading areas and the like, shall comply with the regulations set forth in Section 19.590.070 (Light and Glare) and the provisions of Chapter 19.556 (Outdoor Lighting).
- E.F. *Screening of mechanical equipment.* All roof-supported or ground-supported mechanical equipment and utility equipment shall comply with the regulations set forth in Chapter 19.555 (Outdoor Equipment Screening).
- F.G. *Landscaping.* Front and side yard areas adjacent to streets and interior perimeter landscape planters adjacent to Residential Zones or uses shall be suitably landscaped and continuously maintained as set forth in Chapter 19.570 (Water Efficient Landscaping and Irrigation). Such setbacks shall not be used for off-street parking, loading, storage or accessory buildings.
- G.H. *Performance standards.* All uses shall comply with the performance standards set forth in Chapter 19.590 (Performance Standards) for industrial uses, except that the noise associated with aircraft operations shall be exempt from noise standards but shall comply with any applicable Federal Aviation Administration regulations regarding noise.
- H.I. *Parking and loading requirements.* Parking areas shall be provided as set forth in Chapter 19.580 (Parking and Loading).
- I.J. *Trash receptacles and enclosures.*
1. 4.—All trash storage areas shall be located so as to be convenient to the users and where associated odors and noise will not adversely impact the users.
  2. 2.—The provisions of Chapter 19.554 (Trash/Recyclable Materials Collection Area Enclosures) regarding requirements for the screening of trash receptacles shall apply.

(Ord. 7331 §7, 2016; Ord. 6966 §1, 2007)

#### **19.130.050 - Design review required.**

No new building, structure or sign or exterior alteration or enlargement of an existing building, structure or sign shall be commenced in any industrial zone until design review approval has been granted pursuant to Chapter 19.710 (Design Review).

(Ord. 7331 §7, 2016; Ord. 6966 §1, 2007)

#### **19.130.060 - Other regulations applicable to Industrial Zones.**

In addition to the requirements contained in this chapter, regulations contained in the following titles of the Riverside Municipal Code and other chapters of the Zoning Code may apply.

**Title 5: Business Taxes, Licenses and Regulations**

**Chapter 5.04: Taxes, Licenses and Regulations**

**Chapter 5.16: Close-Out Sales**

~~Chapter 5.24: Dance Halls and Public Dances~~

**Chapter 5.28: Poolrooms**

**Chapter 5.32: Transient Occupancy Tax**

**Chapter 5.38: Pedestrian Food Vendors**

**Chapter 5.48: Swap Meet**

**Chapter 5.52: Massage**

**Chapter 5.60: Bingo**

**Chapter 5.64: Motor Vehicle Fuel Price Posting**

~~Chapter 5.65: Convalescent Transport Vehicles~~

**Chapter 5.66: Ambulances**

**Title 6: Health and Sanitation**

~~Chapter 6.08: Regulation of Food Establishments and Food Facilities~~

~~Chapter 6.09: Regulation of Food Handlers~~

~~Chapter 6.13: Exterior Structure Maintenance~~

~~Chapter 6.14: Landscape Maintenance~~

~~Chapter 6.15: Abatement of Public Nuisances~~

~~Chapter 6.16: Fly Producing Conditions~~

**Title 7: Noise Control**

**Title 8: Animals**

**Chapter 8.18: Commercial Kennels**

**Title 9: Peace, Safety and Morals**

~~Chapter 9.07: Charge for Police Response to Loud or Large Parties~~ **Social Host of Minors Accountability**

**Chapter 9.09: Public Pay Telephone Regulations**

**Chapter 9.16: Litter and Littering**

**Chapter 9.40: Adult-Oriented Businesses**

**Chapter 9.42: Fortunetelling and Occult Arts**

~~Chapter 9.48: Disclosure of Unified Hazardous Materials Programs~~

**Title 12: Airport and Aircraft**

**Title 16: Building and Construction**

**Title 17: Grading**

**Title 18: Subdivision**

**Title 20: Cultural Resources**



(Ord. 7331 §7, 2016; Ord. 6966 §1, 2007)

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* = For CRC, MU-U and MU-V Zones a Site Plan Review Permit (Chapter 19.770) is required for any new or additions/changes to existing buildings or structures.			
** = For a more detailed listing of the permitted land uses in the RA-5 and RC Zones, refer to Sections 19.100.030.A (RA-5 Zone Permitted Uses) and 19.100.030.B (RC Zone Permitted Uses). If any conflict between this Table and Sections 19.100.030.A and 19.100.030.B exists, the provisions of Sections 19.100.030.A and 19.100.030.B shall apply.			
*** = Refer to Chapter 19.149 - Airport Land Use Compatibility and applicable Airport Land Use Compatibility Plan for airport land use compatibility zones where use may be strictly prohibited.			
C = Subject to the granting of a conditional use permit (CUP), Chapter 19.780	DCP = Day Care Permit—Large Family, Chapter 19.860	MC = Subject to the granting of Minor Conditional Use Permit (MCUP), Chapter 19.730	P = Permitted
PRD = Planned Residential Development Permit, Chapter 19.780	RCP = Recycling Center Permit, Chapter 19.870	SP = Site Plan Review Permit, Chapter 19.770	sq. ft. = Square Feet
X = Prohibited			

<sup>1</sup> Commercial Storage Facilities are permitted in all zones with the Commercial Storage Overlay Zone (Chapter 19.190).

<sup>2</sup> Legal, existing duplexes built prior to the adoption of this Zoning Code are permitted in the R-1-7000 Zone see 19.100.060 D.

<sup>3</sup> Allowed with a Planned Residential Development (PRD) Permit, Chapter 19.780.

<sup>4</sup> One single-family detached dwelling allowed on one legal lot 0.25 acres in size or less in existence prior to January 1, 2018 subject to the development standards of the R-1-7000 Zone.

<sup>5</sup> Permitted or conditionally permitted on sites that do not include a residential use.

(Ord. 7462 , § 2(Exh. A), 2019; Ord. 7431 § 3(Exh. A), 2018)

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## Chapter 19.285 - OUTDOOR STORAGE YARD

### 19.285.010 - Purpose.

The purpose of regulating outdoor storage yards is to ensure compatibility of such uses with surrounding uses and properties and to avoid any impacts associated with such uses.

(Ord. 7331 §36, 2016; Ord. 6966 §1, 2007)

### 19.285.020 - Applicability and permit requirements.

Contractor storage yards, as defined in Article X (Definitions), are permitted as set forth in Article V, Base Zones and Related Use and Development Provisions subject to the requirements contained in this chapter.

(Ord. 7331 §36, 2016; Ord. 6966 §1, 2007)

### 19.285.030 - Site location, operation and development standards.

#### A. *Vehicles, equipment and other items customarily stored in outdoor areas.*

1. The storage area and drive aisles shall be paved with not less than two and one-half inches of asphaltic concrete or an equivalent surfacing meeting the established standards and specifications of the Public Works Department, except for any required landscape setback areas as stipulated in item 5 below.
2. All such areas shall be graded and drained so as to dispose of all surface water in a manner consistent with water quality control standards enforced by the Public Works Department.
3. All such areas shall be maintained in good repair in a clean, neat and orderly condition.
4. All such areas shall be provided with internal circulation, safe entrances and exits meeting the established standards and specifications of the Planning Division and Public Works Department.

All such areas shall have a landscaped area not less than ten feet in depth, the depth of the required yard area or the depth as required for specific uses, whichever is the greatest, maintained along the street side of the lot.

(Ord. 7331 §36, 2016; Ord. 6966 §1, 2007)

### 19.285.040 - Screening of outdoor storage.

Screening of outdoor storage shall comply with the following:

- A. Storage shall be visually screened from all adjacent building sites and public streets and alleys by a solid masonry wall of a height sufficient to screen all materials stored outdoors, ~~as determined on a case-by-case basis,~~ or by a building. Such walls shall be limited in height to three feet within the required front or street side yard area, or, where no front or street side yard area is required, such wall shall be limited in height to three feet within ten feet of the street property line. Alternative screening methods including but not limited to fences, landscaping, earthen berms or some combination thereof may be approved by the Community & Economic Development Director or his/her designee provided that the required visual screening is achieved.
- B. The screening herein required shall be established at or before the time any area is used for outdoor storage.

- C. Where topographical conditions or existing structures are such that strict compliance with the requirements of this section would not be necessary to accomplish the purposes of this section, the Approving Authority may waive compliance with all or part of such requirements.

(Ord. 7331 §36, 2016)

19.285.050 - Modifications.

Modifications to the above site location, operation and development standards and screening requirements may be considered in conjunction with a Minor Conditional Use Permit or Conditional Use Permit, as applicable.

(Ord. 7331 §36, 2016)

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## **Chapter 19.435 – WAREHOUSING AND DISTRIBUTION FACILITIES**

### **19.435.010 - Purpose.**

The purpose of regulating warehousing and distribution facilities is to ensure compatibility of such uses with surrounding uses and properties and to avoid any impacts associated with such uses. It is the intent of these regulations to implement the *Good Neighbor Guidelines for Siting New and/or Modified Industrial Facilities* adopted by the City Council MONTH DAY, 2020.

(Ord. XXXX, 2020)

### **19.435.020 – Applicability and permit requirement.**

Warehousing and distribution facilities, as defined in Article X (Definitions), are permitted as set forth in Article V, Base Zones and Related Use and Development Provision, subject to the requirements contained in this chapter.

(Ord. XXXX, 2020)

### **19.435.030 – Site location, operation and development standards.**

The standards set forth in Article V, Base Zones and Related Use and Development Provisions, shall apply to warehousing and distribution facilities, unless otherwise specified here.

#### **A. Warehousing and distribution facilities 10,000 square feet or less.**

1. Driveways, loading areas, docks, truck wells and internal circulation routes shall be oriented away from residential neighborhoods, schools parks, playgrounds, day care centers, nursing homes, hospitals or other public places to the maximum extent feasible.
2. Loading areas, docks, truck wells and outdoor storage areas shall be fully screened from view of residential neighborhoods, schools parks, playgrounds, day care centers, nursing homes, hospitals or other public places and from public rights-of-way with buildings, freestanding walls and fences, landscaping or other means to the satisfaction of the Approving Authority.

Where loading areas, docks, truck wells and outdoor storage areas are located adjacent to a Residential Zone or use, they shall be fully screened from view of the adjacent Residential Zone or use by means of a solid wall with a minimum height of eight feet as measured from the finished grade of the adjacent Residential Zone or use.

3. Operations, including loading, unloading, staging and storage of trucks and trailers, shall comply with Title 7 (Noise) of this Code.

#### **B. Warehousing and distribution facilities larger than 10,000 square feet and less than 100,000 square feet.**

1. Driveways, loading areas, docks, truck wells and internal circulation routes shall be oriented away from residential neighborhoods, schools parks, playgrounds, day care centers, nursing homes, hospitals or other public places to the maximum extent feasible.
2. Loading areas, docks, truck wells and outdoor storage areas shall be fully screened from view of residential neighborhoods, schools parks, playgrounds, day care centers, nursing homes, hospitals or other public places and from public rights-of-way with buildings, freestanding walls and fences, landscaping or other means to the satisfaction of the Approving Authority.

- a. Where loading areas, docks, truck wells and outdoor storage areas are located adjacent to a Residential Zone or use, they shall be screened from view of the adjacent Residential Zone or use by means of a solid wall with a minimum height of eight feet as measured from the finished grade of the adjacent Residential Zone or use.
  3. Within 800 feet of a Residential Zone or use, operations, including loading, unloading, staging and storage of trucks and trailers, shall be limited to between the hours of 7:00 A.M. and 7:00 P.M. Monday through Friday, between the hours of 8:00 A.M. and 5:00 P.M. Saturdays, and shall be prohibited on Sundays and State and Federal holidays unless a Noise Study prepared by a qualified professional can demonstrate that noise related to operations outside these hours will not exceed thresholds established in Title 7 (Noise) as measured from the Residential Zone or use.
  4. Idling of trucks queued or operated on site shall not exceed 5 minutes.
  5. Warehousing and distribution facilities generating more than 50 truck trips per day, as determined by the most recent Institute of Transportation Engineers (ITE) Trip Generation Rate for the specific proposed land use, shall prepare a truck route plan indicating the proposed route to be used by trucks originating from or serving the facility to access the State Highway System. Routes that use local streets or traverse areas zoned or developed with Residential uses should be avoided to the satisfaction of the Traffic Engineering Division of the Public Works Department.
  6. Where transport by temperature-controlled trucks or trailers is proposed, on-site electrical hookups shall be provided at loading docks. Idling or use of auxiliary truck engine power to power climate-control equipment shall be prohibited.
- C. Warehousing and distribution facilities 100,000 square feet and larger.
1. Driveways, loading areas, docks, truck wells and internal circulation routes shall be oriented away from residential neighborhoods, schools parks, playgrounds, day care centers, nursing homes, hospitals or other public places.
  2. Loading areas, docks, truck wells and outdoor storage areas shall be screened from view of residential neighborhoods, schools parks, playgrounds, day care centers, nursing homes, hospitals or other public places and from public rights-of-way with buildings, freestanding walls and fences, landscaping or other means to the satisfaction of the Approving Authority.
    - a. Where loading areas, docks, truck wells and outdoor storage areas are located adjacent to a Residential Zone or use, they shall be screened from view of the adjacent Residential Zone or use by means of a solid wall with a minimum height of eight feet as measured from the finished grade of the adjacent Residential Zone or use.
  3. Sufficient aisle space shall be provided on-site to accommodate the on-site the queuing of trucks as determined by a Traffic Impact Analysis, if required. Queuing lanes or aisles shall not obstruct regular vehicular or pedestrian circulation or emergency equipment access.
  4. Operations, including loading, unloading, staging and storage of trucks and trailers, shall be limited to between the hours of 7:00 A.M. and 7:00 P.M. Monday through Friday, between the hours of 8:00 A.M. and 5:00 P.M. Saturdays, and shall be prohibited on Sundays and State and Federal holidays unless a Noise Study prepared by a qualified

professional can demonstrate that noise related to operations outside these hours will not exceed thresholds established in Title 7 (Noise).

5. Idling of trucks queued or operated on site shall not exceed 5 minutes.
6. A truck and haul route plan shall be prepared indicating the proposed route to be used by trucks originating from or serving the facility to access the State Highway System. Routes that use local streets or traverse areas zoned or developed with Residential uses should be avoided to the satisfaction of the Traffic Engineering Division of the Public Works Department.
7. On-site electrical hookups shall be provided at loading docks. Idling or use of auxiliary truck engine power to power climate-control equipment shall be prohibited.
8. Warehousing and distribution facilities generating 150 or more truck trips per day, as determined by the most recent Institute of Transportation Engineers (ITE) Trip Generation Rate for the specific proposed land use, shall prepare a Health Risk Assessment in accordance with South Coast Air Quality Management District (SCAQMD) Guidelines.

(Ord. XXXX, 2020)

**19.435.040 – Modifications.**

Modifications to the above site location, operation and development standards may be considered in conjunction with the required Minor Conditional use Permit or Conditional Use Permit, as applicable.

(Ord. XXXX, 2020)



## Chapter 19.510 - OUTDOOR STORAGE - INCIDENTAL

### 19.510.010 - Purpose.

This purpose of regulating outdoor storage is to ensure compatibility of such uses with surrounding uses and properties and to avoid any impacts associated with such uses.

(Ord. 7331 §81, 2016; Ord. 6966 §1, 2007)

### 19.510.020 - Applicability and permit requirements.

Outdoor storage - incidental, as defined in Article X (Definitions), is permitted as set forth in Article V, Base Zones and Related Use and Development Provisions subject to the requirements contained in this chapter and the Municipal Code, Title 6.

(Ord. 7331 §81, 2016; Ord. 6966 §1, 2007)

### 19.510.030 - Site location, operation and development standards.

#### A. *Vehicles, equipment and other items customarily stored in outdoor areas.*

1. The lot shall be paved with not less than two and one-half inches of asphaltic concrete or an equivalent surfacing meeting the established standards and specifications of the Public Works Department for a minimum depth measured from all abutting existing or street rights-of-way as follows:
  - a. For the BMP Zone where such activity occurs 100 feet of combined paving and landscaping, with a minimum of ten feet of landscaping or more as may be required by the zone, and 90 feet of paving.
  - b. For the I Zone where such activity occurs 100 feet of paving, with no required landscaping, unless required by the zone.
2. Any outdoor storage area described in Paragraph 1 above shall be paved as provided above or overlaid with a dust-free surface such as decomposed granite, oiled native soil, or a suitable substitute approved by the Public Works Department.
3. All such areas shall be graded and drained so as to dispose of all surface water in a manner consistent with water quality control standards enforced by the Public Works Department.
4. All such areas shall be maintained in good repair, in a clean, neat and orderly condition.
5. All such areas shall be provided with internal circulation, safe entrances and exits meeting the established standards and specifications of the Planning Division and Public Works Department.
6. Where any such area adjoins or is across an alley from property in a zone that permits residential uses, a minimum six-foot high solid decorative masonry wall shall be constructed so as to physically separate the storage area from the residential property. However, such wall shall be limited in height to three feet within the required front or street side yard area, or, where no front or street side yard area is required, such wall shall be limited in height to three feet within ten-10 feet of the street property line.
7. All such areas shall have a landscaped area not less than ten feet in depth, or the depth of the required yard area or the depth as required for specific uses, whichever is the greatest, maintained along the street side of the lot.

(Ord. 7331 §81, 2016; Ord. 6966 §1, 2007)

**19.510.040 - Screening of outdoor storage.**

Screening of outdoor storage shall comply with the following:

- A. ~~When permissible outdoor storage is utilized, such~~ Outdoor storage shall be visually screened from all adjacent building sites, public streets and alleys by a minimum six-foot high solid decorative masonry wall, ~~an alternative decorative screening as defined in Chapter 19.550 (Fences, Walls and Landscape Materials)~~ sufficient to screen all materials stored outdoors, or by a building. Such walls shall be limited in height to three feet within the required front or street side yard area, or, where no front or street side yard area is required, such wall shall be limited in height to three feet within ten feet of the street property line. Alternative screening methods including but not limited to fences, landscaping, earthen berms or some combination thereof may be approved by the Community & Economic Development Director or his/her designee provided that the required visual screening is achieved. ~~However, the wall shall be limited in height to three feet within the required front or street side yard area, or, where no front or street side yard area is required, such wall shall be limited in height to three feet within ten feet of the street property line.~~
- B. The screening herein required shall be established at or before the time any area is used for outdoor storage.
- C. Where topographical conditions or existing structures are such that strict compliance with the requirements of this section would not be necessary to accomplish the purposes of this section, the Approving Authority may waive compliance with all or part of such requirements.

(Ord. 7331 §81, 2016; Ord. ~~5968~~ §1, 2017)

**19.510.050 - Modifications.**

Modifications to the above site location, operation and development standards and screening of outdoor storage standards shall require consideration of a Minor Conditional Use Permit.

(Ord. 7331 §81, 2016)

## Chapter 19.550 - FENCES, WALLS AND LANDSCAPE MATERIALS

### 19.550.010 - Purpose.

This chapter sets forth standards for the construction and maintenance of fences, walls, and landscape materials to ensure that such features are aesthetically pleasing and can provide for privacy and safety without obstructing views and without creating a public safety hazard or nuisance.

(Ord. 7331 §88, 2016; Ord. 6966 §1, 2007)

### 19.550.020 - Prohibited materials.

Fences or walls containing razor wire (visible to a public right-of-way, alley or parking lot), barbed wire or electrified wire shall be prohibited unless a variance is granted pursuant to Chapter 19.720 (Variance).

(Ord. 7331 §88, 2016; Ord. 6966 §1, 2007)

### 19.550.030 - Height and location provisions.

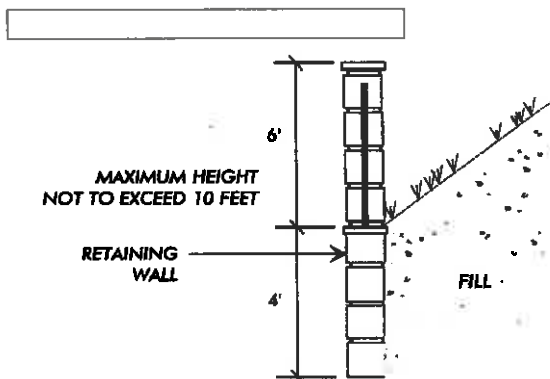
Fences, walls, and hedges shall be allowed in conformance with the following provisions:

#### A. Front yards.

1. In the RA-5, RC, RR and RE Zones, any fence or wall in the required front yard setback may be up to six feet in height, provided that the openwork portion of the fence or wall above a height of three feet shall be no more than one part solid to three parts open with no portion of the solid wall, excluding pilasters, extending above three feet.
2. In all other zones, front yard fences or walls shall not exceed four feet in height provided that the openwork portion of the fence or wall above a height of three feet shall be no more than one part solid to three parts open with no portion of the solid wall, excluding pilasters, extending above three feet.
3. The height of fences or walls in front yard areas shall be measured inclusive of retaining wall portion.

#### B. Side and rear yards.

1. A fence or wall along a side or rear property line may be up to six feet in height provided it does not extend into a front yard. Higher fences or walls in commercial or industrial zones may be required by other provisions of the Zoning Code.



**19.550.030.B.2 Height of Retaining Walls**

2. In side and rear yard areas, the height of fences or walls may be increased in height by up to a maximum of four feet by retaining wall portions.
3. The fencing around tennis courts along rear and interior side yards shall not exceed 12 feet in height and must be partially open above six feet in height subject to approval of the Community & Economic Development Director or his/her designee.

**C. All yards.**

1. All height restrictions applying to fences and wall shall apply equally to hedges planted within required yards forming a barrier serving the same visual purpose as a fence or wall.
2. Notwithstanding A and B above, any minimum required height of walls established by the Zoning Code for screening or safety purposes shall be measured from the highest grade, not including retaining wall portion.

**D. Exceptions.**

1. Alternative decorative screening constructed of a material other than solid decorative masonry may be permitted where not visible from the public right-of-way subject to the approval of the Community & Economic Development Director or his/her designee.
- 4-2. Fences or walls around a public utility building or structure may exceed the provisions of this section as needed for security or public safety, subject to approval of the Community & Economic Development Director or his/her designee.
- 2-3. Outdoor storage yards and incidental outdoor storage shall be subject to the location and design regulations of Chapter 19.285 (Outdoor Storage Yard) and Chapter 19.510 (Outdoor Storage - Incidental).

(Ord. 7331 §88, 2016; Ord. 6966 §1, 2007)

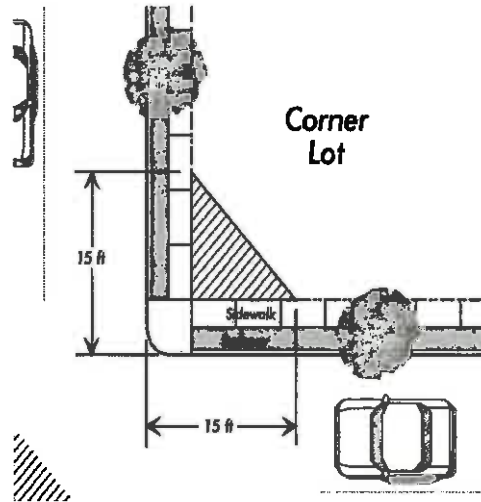
**19.550.040 - Fences, walls and hedges not in compliance.**

Any fence, wall or hedge that does not comply with this chapter is not permitted unless a variance is granted pursuant to Chapter 19.720 (Variance).

(Ord. 7331 §88, 2016; Ord. 6966 §1, 2007)

**19.550.050 - Sight clearance requirements.**

To safeguard against vehicle, bicycle, and pedestrian collisions caused by visual obstructions at street and/or alley intersections, a clear cross-visibility area shall be maintained at the intersection of the public rights-of-way, unobstructed by any fence or wall taller than three feet above the street grade. At any corner formed by the intersection of two streets and/or alleys, the required clear cross-visibility area shall be a triangle with two street sides 15 feet long extending along the curb line of each street and/or alley.



Nothing shall be placed here above a maximum 42' height.

**19.550.050 Clear Visibility Triangle**

(Ord. 7331 §88, 2016; Ord. 7235 §10, 2013; Ord. 6966 §1, 2007)

**19.550.060 - New residential construction**

For any new construction of a single-family residence, any wall along a street rear yard, street side yard or front yard between the house and the side yard shall be constructed with of a decorative masonry or a similar material subject to the approval of the Community & Economic Development Director or his/her designee. Other type fencing is not permitted in these areas. This provision does not apply to interior rear or interior side yard fences and walls.

(Ord. 7331 §88, 2016; Ord. 6966 §1, 2007)

**19.550.070 - Historic properties and cultural resources.**

Fences, walls, and landscape materials for cultural resources shall be designed to comply with Title 20 (Cultural Resources) of the Riverside Municipal Code, the Cultural Heritage Board's Design Guidelines, and district-specific guidelines.

(Ord. 7331 §88, 2016; Ord. 7109 §10, 2010)

## ARTICLE X: DEFINITIONS

### Chapter 19.910 - DEFINITIONS

#### 19.910.240 - "W" Definitions.

*Wall* means a physical barrier constructed largely of masonry, brick, concrete, stucco, concrete block or any combination thereof and intended to mark a boundary and/or enclose an area.

*Wall, building.* See "building wall."

*Wall, parapet.* See "parapet wall."

*Warehouse* means a building or portion thereof used for the storage, receiving, shipping, or wholesaling of goods and merchandise, and any incidental or accessory activities. A warehouse is not "commercial storage" as defined in this title.

*Warehousing and distribution facility* means a building, structure or other facility primarily used for the warehousing, storage, sorting, packing, staging, shipping, receiving, distributing or holding of goods and merchandise of any kind, typically which are not manufactured, processed or otherwise produced in the same facility, for any length of time. Warehousing and distribution facilities include but are not limited to wholesale distribution, distribution centers, moving and transfer storage, cross-dock facilities, package handling centers, order fulfillment centers, or logistics centers and facilities.

*Wet bar* means any room or area of a room used, intended or designed to be used, for the occasional preparation of food. A wet bar may include a single sink or basin, bar height refrigerator, bar height cabinets and hookups for cold water. This definition does not include amenities such as multiple basin sinks, full height refrigerators, hot water hookups, 220 amp electrical service, above counter cabinets, cooking apparatus including, but not limited to, stoves, ranges, hot tops, and microwaves, or any other amenities that would comprise a kitchen.

*Wholesale* means the sale of goods by bulk for resale purposes and not for direct use or consumption.

*Wine.* See "brewery, wine."

*Winery.* See "brewery, winery."

*Worm farm* means the growing of earthworms for commercial or noncommercial purposes in worm beds or other delineated areas or structures, and the use of certain appurtenant structures such as sunshades and packing shades that are utilized in the operation of a worm farm.

(Ord. 7331 §128, 2016; Ord. 7235 §26, 2013; Ord. 7185 §8, 2012; Ord. 7105 §10, 2010; Ord. 6966 §1, 2007)

**P20-0190**

**Hunter Business Park Specific Plan Amdt.**

### **III. DEVELOPMENT STANDARDS AND DESIGN GUIDELINES**

The Development Standards and Design Guidelines for the Hunter Business Park Specific Plan consist of two sub-sections:

1. Permitted Uses—; an outline of typical uses permitted in each land use district. These uses are consistent with the existing zoning of each area.
2. Development Standards and Design Guidelines—; special design considerations which are not addressed in the Riverside Municipal Code.

The standards and guidelines have evolved from consideration of goals and objectives for Hunter Business Park, analysis of existing conditions, and consideration of the Industrial Market Analysis and Economic Development Strategy for Riverside prepared by Williams Kuebelbeck & Associates.

These standards, together with current city zoning standards set forth in Title 19 of the Riverside Municipal Code (Title 19), are intended to implement the Hunter Business Park Specific Plan. They are intended to be utilized by the City, developer and builder to ~~insure~~ensure that proposed development will proceed in an efficient and coordinated manner, to create a high-high-quality industrial area. The standards and guidelines are designed to be compatible with the three major land use districts in the area.

#### **A. PERMITTED USES**

The proposed Land Use Districts discussed in Chapter II define a generalized land use character for each of the three districts in the Hunter Business Park area. These three districts are the General Industrial District, the Garden Industrial District and the Industrial Park District.

Within each of the three land use districts, Title 19 ~~of the Riverside Municipal Code~~ permits or prohibits certain uses in certain zones. Exhibit III-1: Existing Zoning illustrates the existing zoning in the area at the time this Specific Plan was adopted. The zones which ~~exist~~ existed at the time of Plan adoption in each district ~~are~~ were as follows:

##### **General Industrial District**

The General Industrial District ~~is~~ was primarily zoned for Manufacturing Park (MP), Light Manufacturing (M-1), and General Manufacturing (M-2) uses. There ~~are~~ were several small areas zoned for Restricted commercial (C-2), General Commercial (C- 3), Restricted Office (R-O) and Residential (R-1). In addition, a Business Park (BP) combining zones ~~exists~~ exists in certain parts of this district. In the case of this district, this zone ~~combines~~ combined with an MP zone to permit a limited range of retail commercial uses having a close association with, providing convenience to, or which are compatible with office, wholesale, warehousing and manufacturing uses permitted in these zones.

##### **Garden Industrial District**

The Garden Industrial District ~~is~~ was zoned only for Manufacturing Park (MP) uses. A total





of about 71 acres were zoned RA -Residential Agricultural pursuant to Measure C, however, their zoning was changed to MP under the adoption of Case R-20--890/SPA-3-890 in 1990.

### **Industrial Park District**

The Industrial Park District ~~is~~was zoned for Manufacturing Park (MP) uses with a residential (R-1) infill area north of the Springbrook arroyo.

The uses which are permitted within the zones of the district establish the land use character of the district. Because there are a number of different zones in the General Industrial District, this district will be characterized by a wide variety of uses and use types. Both the Garden Industrial and Industrial Park Districts have the same zoning, Manufacturing Park (MP), thus their uses will be ~~similar~~similar, and character of these areas will be determined by factors other than land use. For example, utilization of the ~~5-5~~-acre minimum parcel size for initial planning and development purposes in the Industrial Park district will enhance internal project consistency from a planning, circulation and site design perspective.

Title 19 ~~of the Riverside Municipal Code~~ prevails in all land use decisions for the Hunter Business Park Specific Plan area. The Specific Plan cannot permit any uses in the area which are prohibited by Title 19. The Specific Plan can, however, be more restrictive than Title 19 and prohibit uses which would normally be permitted under the existing zoning. Thus, the Specific Plan is a tool for establishing the desired land use and development character for each district.

Table ~~2~~III-1, the Land Use Matrix uses the three land use districts and the existing district zoning to identify the uses which are permitted within the Specific Plan area.

### **B. PERMITTED USES**

Uses permitted within each district are those permitted by Title 19 ~~of the Riverside Municipal Code with some limitations and restrictions as provided for in this Specific Plan document. These uses are indicated by a "P" in~~except as modified by Table III-1: Land Use Matrix.



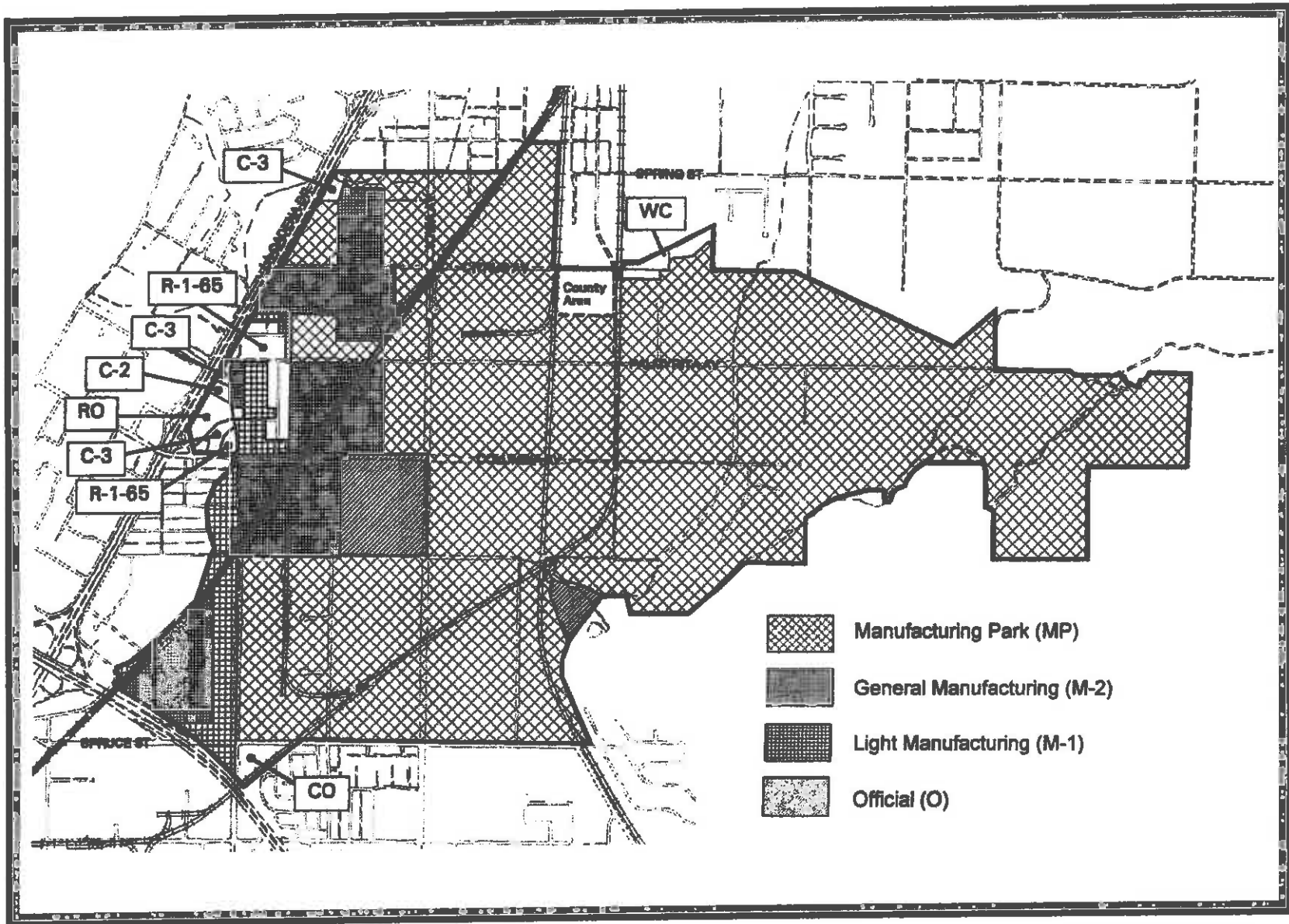


Figure III — 1 Existing Zoning (Plan Adoption)

PERMITTED USES	General Industrial District								Garden Industrial District		Industrial Park District
	MP-BP	MP	M-1	M-2	C-2	C-3	RO	R1	MP	MP	
<b>INDUSTRIAL/MANUFACTURING</b>											
Wholesale Distribution	P/CUP	P/CUP	P/CUP	P/CUP					P/CUP	P/CUP	
Warehousing/Storage	P/CUP	P/CUP	P/CUP	P/CUP					P/CUP	P/CUP	
Manufacturing	P	P	P	P					P	P	
Research and Development	P	P	P	P					P	P	
Publishing and Printing	P	P	P	P					P	P	
Ancillary Retail Sales On-Site	P	P	P	P					P	P	
Off-Site Retail Sales	P	P	P	P					P	P	
Wholesale Showrooms	P	P	P	P					P	P	
<b>OFFICE</b>											
Office and Administrative	P	P	P	P	P	P	P		P	P	
<b>RETAIL</b>											
Convenience Sales					P	P					
Eating and Drinking (part of a multi-tenant)	P	P	P	P	P	P			P	P	
Eating and Drinking (freestanding building)				P	P	P					
Business Supply Retail	P			P	P	P					
Building Supplies and Sales				P	P	P					
<b>SERVICES</b>											
Automotive Rental					P	P					
Automotive Service Station	CUP	CUP	CUP	CUP	CUP	CUP					
Building Maintenance Services			P	P		P					
Business Support Services	P				P	P	P				
Heavy Equipment Sales and Rental						P					
Medical/Health Care Services	P	P	P	P	P	P			P	P	
Personal Services	P				P	P					
Recreational Facilities	CUP	CUP			CUP	P			CUP	CUP	
Repair Services	P		P	P			CUP**		CUP	CUP	
<b>DESIGN STANDARDS FOR SPECIFIC PLAN AREA</b>											
Minimum Parcel Size ****	Per Underlying Zone								One Acre		Five Acres (One Acre after Approved Master Plan)
Minimum Lot Width ****	Per Underlying Zone								140 Feet		300 Feet (140 Feet after Approved Master Plan)
Building Site Coverage	Not Greater than 50%								Not Greater than 50%		Not Greater than 50%
Building Height	As Defined for Each Parcel per the Existing Zoning								45 Feet		45 Feet
Building Setbacks from Major Secondary Street	Average 50 Feet; Minimum 40 Feet								Average 50 Feet; Minimum 40 Feet		Average 50 Feet; Minimum 40 Feet
Building Setbacks from Local Streets	Per Underlying Zone								Per Underlying Zone		Per Underlying Zone
Interior Building Setbacks	Side and Rear: Minimum 20 Feet								Side and Rear: Minimum 20 Feet		Side and Rear: Minimum 20 Feet
Parking Setbacks ***	20 Feet All Zones								20 Feet		20 Feet
Parking Standards	To Be Determined by Use Within Structure (Per Title 19)								To Be Determined by Use Within Structure (Per Title 19)		To Be Determined by Use Within Structure (Per Title 19)
Tenancy	Multiple Tenant Parcels Permitted								Multiple Tenant Parcels Permitted		Multiple Tenant Parcels Permitted
<b>NOTES</b>											
P = Permitted Use with Design Review Approval      CUP = Conditional Use Permit with Approval by Planning Commission or Minor Conditional Use Permit with approval by Development Review Committee, per Title 19      Signs per Title 19											
P/CUP = Permitted Use, Minor Conditional Use Permit or Conditional Use Permit based on square footage, per Title 19											
Certain Uses Normally Allowed in the Zoning Classifications above are Precluded in this Specific Plan Area Business Support Retail Overlay Zone may be applied to a specific property subject to the approval of a Rezoning request, in accordance with Chapter 19.810 of the Riverside Municipal Code. See Section III-F of the Specific Plan for a list of Permitted and Conditionally Permitted uses in the BSR Overlay Zone.											
** — Permitted Within a Completely Enclosed Building      *** — Columbia, Marlborough, Service, Town, Chicago and Columbia Lake Estates of Iowa											

\*\*\*\* -- Parcels approved under a master plan and associated design guidelines as outlined in 1.1 may be less than one acre with widths less than 140 feet per City Planning Commission approval. In addition, a front yard setback of twenty feet is permitted and interior

### Table III -- 1: Land Use Matrix

### C. CONDITIONAL USES

All uses permitted within Hunter Business Park are uses permitted in the area by Title 19 of the ~~Riverside Municipal Code~~. The Specific Plan shall not permit any uses in the Hunter Business Park area which are not presently permitted under Title 19. The Hunter Business Park Specific Plan may, however, be more restrictive in these districts and prohibit uses which are permitted by Title 19.

Conditional uses are those land uses which are deemed to possess location, use, building, or traffic characteristics of such unique and special form as to make impractical or undesirable their automatic inclusion as permitted uses in certain zones. Development projects requiring a Conditional Use Permit or Minor Conditional Use Permit shall be required to comply with the regulations of ~~Section 19.64.010~~ of Title 19 of the ~~Riverside Municipal Code~~. The ~~Planning Commission~~ Approving Authority shall make the following findings before the granting a Conditional Use Permit or Minor Conditional Use Permit:

1. That the proposed use will comply with the purposes of the General Land Use designation in which it is located including all provisions of the General Plan and Specific Plan, if applicable.
2. That the proposed use, together with conditions applicable thereto, will not be detrimental to the public health, safety and welfare or injurious to properties or improvements in the area.

These uses are indicated by a "CUP" in Table III-1: Land Use Matrix.

### D. USE RESTRICTIONS

The following restriction shall apply to all properties in the Specific Plan area:

1. No uses shall be permitted which will emit any offensive odor, dust, noxious gas, noise, vibration, smoke, heat, or glare beyond the parcel wherein such use is located. All uses shall comply with standards of performance, ~~Section 19.46.120 a-n~~ of Title 19 ~~Riverside Municipal Code~~.

### E. DEFINITIONS

The permitted uses Shown in Table III-1 are defined as set forth in Article X (Definitions) of the Zoning Code, except as modified as follows:

#### **Industrial/Manufacturing/Wholesale**

Industrial uses include the manufacture, assembly, fabrication, warehousing and wholesale distribution of goods.

~~Wholesale distribution~~ — ~~Activities include, but are not limited to, the distribution of goods in large quantities for resale.~~

~~Warehousing/Storage — Activities include the storage of goods in a space specifically intended for this purpose.~~

Manufacturing — Activities typically include, but are not limited to, the assembly fabrication or manufacturing of furniture, food products, garments, signage, electrical appliances and toys. (The manufacturing of fish, meat, sauerkraut, and vinegar products and the rendering or refining of fats and oils is prohibited.) Prior to approval of the manufacturing of any product using yeast the applicant shall provide documentation, from an individual qualified in odor control and measurement, that the process proposed does not produce odors that will be noticeable beyond the subject property boundaries and the Best Available Control Technology (BACT) shall be used to reduce the Volatile Organic Compound (VOC) emissions to a level acceptable by the South Coast Air Quality Management District (SCAQMD).

Showrooms — Display of goods/merchandise in conjunction with a permitted wholesale, distribution or industrial business and limited to products and services associated with the permitted land use.

Research and Development — Activities typically include, but are not limited to, scientific research and theoretical studies and investigations, including the fabrication and testing of prototypes, and the performance of environmental tests, and related activities, by or under the supervision of professional scientists and highly trained specialists in the field of physical, economic, or social research.

Incubator Industry — Business enterprise characterized by minimal building space needs and employees.

### **Publishing and Printing**

### **Office**

Administrative and Office: Activities typically include, but are not limited to, professional, executive, management, or administrative uses of private, profit-oriented firms others than public utility firms. Uses typically include, but are not limited to, administrative, legal, medical, financial and insurance offices, and architectural firms. Any drive-up service is specifically excluded.

### **Retail**

Ancillary Retail Sales On-Site: Activities include direct retail sales to the public on-site of goods or services that are ancillary to the primary use and operation of the facility. Ancillary retail uses shall include the sale of products manufactured, assembled, fabricated or processed in whole or major part on premises. Floor area allocated for retail sales shall not exceed more than fifteen percent of the total building(s) gross floor area, up to a maximum of 7,500-square-feet. Off-street parking requirements for commercial uses shall be applied



to the portion of the facility dedicated for on-site retail sales in compliance with the Zoning Code (19.74580).

Off-Site Retail Sales: Activities include direct retail sales that require no general public retail sales on-site (e.g., direct mail merchants, mail order catalog houses, wholesale distributors, sales of service, products and material to service customers off-site, and direct retail sales of merchandise and products that occur off-site.)

Building Supplies and Sales: Activities typically include, but are not limited to, the retail sale or rental, from the premises, of goods and equipment, including paint, glass, hardware, fixtures, electrical supplies and lumber, and hardware stores and may have outdoor storage where allowed.

Business Supply Retail: Activities typically include, but are not limited to, retail sales, rental, or repair from the premises, of office equipment, office supplies and similar office goods, primarily to firms and other organizations utilizing the goods, rather than to individuals. They exclude the sale or rental of motor vehicles and the sale of materials used in construction of buildings or other structures.

Convenience Sales: Activities typically include, but are not limited to, the retail sales from the establishments or places of business primarily engaged in the provision of frequently or recurrently needed small personal convenience items. These include various general retail sales and personal service of an appropriate size and scale to meet the above criteria. Uses typically includes, but are not limited to, beauty and barber shops, apparel laundering and dry-cleaning establishments, and neighborhood grocery stores.

Eating and Drinking Establishments: Activities typically include, but are not limited to, establishments or places of business primarily engaged in the sale of foods and beverages.

Eating and Drinking (part of a multi-tenant industrial park): Restaurants, cafes and cafeterias, including those with outdoor dining ~~as defined in Section 19.04.394 of Title 19,~~ operated in conjunction with and incidental to a single permitted use for the convenience of persons employed upon the premises or operated within a multi-tenant industrial park for the convenience of persons employed within the same industrial park, each established and operated in accordance with the following criteria:

- A. A permitted restaurant, cafe or cafeteria shall be located within the building of a single permitted use to serve the employees of that use only, or shall be located in a tenant or owner space in a multitenant industrial park developed with attached tenant or owner spaces to primarily serve the employees of that multitenant industrial park. This use shall not be permitted as the sole or primary use in a separate freestanding building;
- B. A permitted restaurant, cafe or cafeteria located in a tenant or owner space in a multitenant industrial park shall not exceed one thousand five hundred square feet in total area including kitchen, storage, restroom and outdoor dining areas. A permitted

restaurant, cafe or cafeteria located within the building of a single permitted use is not restricted to a maximum size;

- C. The menu shall be limited to breakfast, lunch and snack items only;
- D. The hours of operation shall be limited to six a.m. to six p.m., Monday through Friday; and
- E. Parking shall be established and maintained in accordance with ~~subsection O of Section 19.74.010~~ Chapter 19.580 of Title 19.

Eating and Drinking (Freestanding, Sit-down, Building): Restaurants, cafes and cafeterias, including those with outdoor dining and food preparation ~~as defined in Section 19.04.394 of Title 19~~. Outdoor dining and/or food preparation shall be subject to the requirements, as applicable, ~~of Section 19.26.020, subsections (G) and~~

~~(H) of Title 19~~. Alcoholic beverage sales shall be ~~limited to the incidental sale of beer and wine only~~, subject to the granting of a minor conditional use permit and the site location criteria, operation standards, development standards and exemptions of ~~Section 19.26.020, subsection J of Title 19~~.

### **Service**

Automotive Rental: Activities typically include, but are not limited to, the rental, from the premise, of motor vehicles. Uses typically ~~include, but include but~~ are not limited to car rental agencies.

Automotive Service Station: Activities typically include, but are not limited to, the sale from the premises of goods and the provision of services normally required in the day-to-day operation of motor vehicles, including the principal sale of petroleum products, the incidental sale of tires, batteries, replacement items, lubricating services, and the performance of minor repairs, such as tune-ups, tire changes and brake work, as well as convenience markets in conjunction with the service station all subject to a conditional use permit.

Building Maintenance Services: Activities typically include, but are not limited to, maintenance and custodial services, window cleaning services, disinfecting and exterminating services, and janitorial services.

Business Support Services: Activities typically include, but are not limited to, firms rather than individuals engaged in the provision of services of a clerical, employ-ment, protective, or minor processing nature, including multi-copy and blue-print services. They exclude the printing of books, other than pamphlets and small reports for another firm, and where the storage of goods other than samples is prohibited.

Typical uses include secretarial services, telephone answering services, and blueprint services.

Heavy Equipment Sales and Rental: Activities include, but are not limited to, the sale or



rental from the premises of heavy construction equipment, trucks, and farm equipment together with maintenance. Uses typically include, but are not limited to, heavy trucks and construction equipment dealers.

Medical/Health Care Services: Activities typically include, but are not limited to, establishments primarily engaged in the provision of personal health services including prevention, diagnosis and treatment or rehabilitation services provided by physicians, dentists, nurses and other health personnel as well as the provision of medical testing and analysis services, but excludes those classified as any public use type. Typical uses include medical offices, dental laboratories and fitness or wellness clinics.

Personal Services: Activities typically include, but are not limited to, information, instruction and similar services of a personal nature. Uses typically include, but are not limited to, driving schools, day care facilities, travel bureaus, and photography studios.

Recreational Facilities: Activities typically include, but are not limited to, sports performed, either indoor or outdoor, which require a facility for conducting the recreational activity. Uses typically include, but are not limited to, tennis courts, sports fields, and golf courses.

Repair Services: Activities typically include, but are not limited to, repair services involving articles such as upholstery, furniture, and large electrical appliance.

### **City/Public Use**

Public use types include the performance of utility, educational, recreational, cultural, medical protective, government, and other uses which are strongly vested with public or social importance.

Civic Services - Activities typically include, but are not limited to, management or administrative services performed by public, quasi-public, and public, governmental or utility administrative offices. Uses typically include, but are not limited to, public parks, and open space areas of an active or passive character, playgrounds, playing fields, and community halls.

Utility Facilities/Services - Activities typically include, but are not limited to, those performed by public agencies which are strongly vested in the public interest.

### **Prohibited Uses**

Uses prohibited within Hunter Business Park are: automobile impound yards (M-1); feed and fuel yards (M-1), and poultry and rabbit killing (M-1).

### **Hunter Business Park Business Support Retail “BSR” Overlay District**

The Business Support Retail Overlay District (BSR Overlay) is primarily intended to allow for support retail uses in areas generally located along arterial streets within ~~centralized~~ centralized locations accessible to the industrial business and visitors doing

business in the Hunter Park. The application of the BSR Overlay shall require a Rezoning request, in accordance with Chapter 19.810 of the Municipal Code.

The following uses shall be allowed in addition to all other uses permitted by the underlying land use district within the Hunter Business Park Specific Plan:

1. Office Supply Retail
2. Cell Phone Retailers
3. Computer Sales/Repair
4. Banks and Financial institutions
5. Dry Cleaning
6. Shoe Repair
7. Florist
8. Postal Services
9. Bakery
10. Beauty/Barber Shop
11. Day Spa
12. Medical Supplies
13. Photographic/Camera Store
14. Nail and Tanning Salons
15. Small fitness facilities, not more than 4,000 square feet in size
16. Blueprint Store
17. Tailor Shop
18. Weight Loss Centers
19. Restaurants, excluding drive-thru
- ~~20. Medical Supply Sales~~
- ~~21. Photographic/Camera Shops~~
- 22.0. Other similar uses, subject to the approval of the Planning Director

(Resolution No. 21797, 2009)

The following uses shall be Conditionally Permitted:

1.      Drive-thru restaurants

(Resolution No. 23009, 2016)

## **F. DEVELOPMENT STANDARDS**

Development Standards for Hunter Business Park are intended to define and emphasize the uniqueness of the project area and to establish a ~~high-high~~-quality level of development. This will be accomplished by providing a framework of continuity and consistency through development.

The development of each parcel within Hunter Business Park is controlled and restricted by these Development Standards and Design Guidelines and must also meet all requirements of the City of Riverside as well as applicable governmental codes and regulations.

The development standards for the Hunter Business Park area address general provisions which include:

- Lot standards
- Utility service
- Building Material Requirements
- Signs
- Circulation
- Design Review

#### 1.0 General Provisions

The purpose of standards within General Provisions is to establish minimum standards regulating specific details in the development of any project within the Hunter Business Park area. Development standards not specifically prescribed by the Specific Plan shall be governed by the appropriate provisions of Title 19 (Zoning) of the Municipal Code according to the underlying Zoning designation of the site.

##### 1.1 Lot Standards

The purpose of lot standards is to establish adequate development sites so that appropriate site planning criteria (i.e., parking, ingress and egress) can be accommodated. Table III-1: Land Use Matrix, summarizes the lot standards for the three land use districts.

Minimum parcel size shall be one (1) acre in the ~~General Industrial and Garden Industrial~~ Districts.

Minimum parcel size shall be five (5) acres or the existing legally established lot size in the Industrial Park District for planning approval and overall development purposes. The overall development requirement is based on the concept of a unified architectural theme along with mutual parking and access agreements. Following approval of an overall development plan these parcels could be divided into parcels as small as one (1) acre 40,000 square feet parcels for sales or leasing purposes. The standards of the BMP — Business and Manufacturing Park Zone shall apply for land division and development purposes unless specifically modified by this Specific Plan. Industrial condominiums shall be permitted to accommodate leases or sales within the framework of an approved overall development

plan.

Except along designated major arterials, the above minimum parcel size in the Industrial Park District may be reduced to a minimum of 0.5 acre if a master plot plan and design guidelines are submitted and approved by the ~~Planning Commission~~ Approving Authority in conjunction with a subdivision map. The master plot plan shall clearly indicate all site elements, including building locations, access, circulation, parking, landscaped areas, fences/walls, walkways, loading zones, storage areas, trash enclosures, lighting, and the like. The design guidelines shall address architecture, signage, screening, landscaping, fences/walls, and lighting as required for all other development proposals by this chapter. In addition, CC&Rs, to establish the design guidelines and other appropriate development criteria, shall be required for all projects approved under this provision. ~~Additional Planning Commission approval is not required for development on individual parcels as long as the development is in substantial compliance with the approved master plot plan and design guidelines.~~

#### Minimum Lot Width

Minimum lot width shall be 140 feet in the ~~General Industrial and Garden Industrial~~ Districts, and 300 feet in the Industrial Park District unless otherwise approved by the Planning Commission as part of a master plan.

#### Building Site Coverage

~~Building site coverage shall not exceed fifty (50) percent in all districts.~~ Building Height

Maximum building heights shall be as ~~defined for each parcel in the General Industrial District per the existing zoning district. (set forth in Title 19) —of Riverside Municipal Code).~~ Maximum building heights shall be 45' in the ~~Garden Industrial and Industrial Park~~ for all Districts.

#### Building Setbacks

#### Front Yards

For arterial streets and buildings over 30 feet in height: A front yard having an average depth of 50 feet but in no case less than 40 feet except as indicated below. The front 20 feet of such front yard shall be suitably landscaped with plant materials and adequately maintained.

Notwithstanding any other provision of this Title to the contrary, pedestrian walks, vehicular access drives and railroad tracks which are perpendicular to the front property line in the front twenty feet, meter pits and utility manholes extending not more than six inches



above finished grade, light fixtures and any recording instruments required by this Chapter shall be permitted in any front yard. Off-street parking, gate or guard houses, roofs or canopies covering unenclosed pedestrian walks and walls or fences not more than six feet in height shall be permitted in the rear 30 feet of any required front yard. A front yard having a minimum depth of forty feet shall be permitted when such yard is landscaped in its entirety, except that a driveway parallel to the front property line a maximum of 12 feet in width may be located within this landscaped front yard;

For all other streets where the building is less than 30 feet in height: A front yard having a depth of 20 feet. The entirety of which shall be suitably landscaped with plant materials and adequately maintained. Notwithstanding any other provision of this Title to the contrary, pedestrian walks, vehicular access drives and railroad tracks which are perpendicular to the front property line in the front 20 feet, meter pits and utility manholes extending not more than six inches above finished grade, light fixtures and any recording instruments required by this Chapter shall be permitted in any front yard.

Parcels containing less than one acre as approved under a master plan and associated design guidelines as outlined under 1.1 above shall have a minimum front yard setback of twenty feet.

~~Side yards shall not be required, except that minimum side yards of 50 feet shall be required wherever a lot or parcel in the MP zone abuts a lot or parcel in any R zone. In the event a side yard abuts a street, such side yard shall meet all of the minimum requirements for a front yard and may be used in accordance with the front yard provisions pertaining to permitted uses.~~

~~Rear yards shall not be required, except that a minimum rear yard of 50 feet shall be required wherever a lot or parcel in the MP zone abuts a lot or parcel in any R zone. In the event a rear yard abuts a street, such rear yard shall meet all of the minimum requirements for a front yard and may be used in accordance with the front yard provisions pertaining to Side Yards~~

~~permitted uses. Side and rear yard setbacks shall be required as set forth in Title 19 of the Riverside Municipal Code based on the underlying zoning designation and the intended use or development of the site.~~

~~Parcels containing less than one acre as approved under a master plan and associated design guidelines as outlined under 1.1 above shall have a minimum setback of twenty feet.~~

### Parking Setbacks

Parking setbacks from major arterials shall be 20 feet from the right-of-way. All other minimum requirements for parking and landscaping setbacks shall be consistent with standards required in the Zoning Code (19.74) for the underlying zone. The parking setback shall be considered part of the total building setback-provided that the average building setback is



maintained according to the street classification.

(Resolution No. 20436, 2003)

### Tenancy

~~Multiple tenant parcels shall be permitted in the General Industrial and Garden Industrial Districts. Large single tenant users shall be encouraged in the Industrial Park District, but not required.~~

### 1.2 Building Materials

Except within the areas northerly of Columbia Avenue and westerly of Iowa Avenue, the use of prefab metal, such as roll formed metal siding or corrugated metal, shall be prohibited from use within the Hunter Business Park Specific Plan boundaries. This does not preclude the use of metal detail as part of architecturally designed buildings. (An example of this would "Corten" steel, a steel which is designed so the outside surface oxidizes.) Where permitted, metal buildings (northerly of Columbia and westerly of Iowa Avenue) are subject to the following:

- Metal buildings shall be permitted as accessory buildings and not as a primary building;
- The new metal building shall be of a high quality appearance;
- The new metal building shall be located no closer than 50 feet to the primary street frontage and no closer than the existing primary building; and
- Landscaping or other screening may be required.

### 1.3 Signs

Signage shall follow the requirements set forth in Chapter 19.76 of Title 19 of the Riverside Municipal Code.

### 1.4 Utilities

The purpose of the utility service standard is for the efficient distribution of utilities designed to be compatible with the surrounding environment. The following shall apply to the Hunter Business Park Specific Plan area:

- All existing and new utilities 12kv or less within the project area along adjacent major arterials (Columbia, Iowa, Marlborough and Spruce Avenues) shall be installed underground. Funding for the undergrounding of these lines shall be accomplished by means of an assessment district as provided for in Chapter IV: Implementation. All 69kv lines are required to remain above ground. Other lines on the 69kv poles shall be undergrounded.
- For subdivision approvals the installation of cable conduits in the public right-of-way is required to the Public Works and Public Utilities Departments.

## 1.5 Circulation

### G. ACCESS

Circulation standards are intended to assure the effectiveness, capacity and safety of the roadways.

- Access to arterials shall be granted to ~~parcels, but parcels~~ but will be limited to one access point per 300 feet of frontage for parcels with greater than 300 feet of frontage, or one access point per parcel with less than 300 feet of frontage. For example, for 250 feet of frontage, one access point shall be permitted; for 750 feet of frontage, two access points shall be permitted.
- No development shall be permitted on parcels without improved street access. For purposes of this standard, a street is a maintained public street or alley. A private street is subject to the approval of the City Planning and Public Works Department.
- Access points, wherever possible, shall be located a minimum of 200 feet from the back of curb returns at intersections, on four (4) lane or wider roads (88' and 110' streets).

### H. RIGHT-OF-WAY

- Dedication of the right-of-way, as shown on the City's Circulation/Transportation Planned Street Lines or in this Specific Plan, shall be required as a condition of approval for all development.

### I. LOCAL STREETS

Local streets, within the project area, shall connect planning areas with the arterial roadways. Local streets within the project area shall have a sixty-six (66) foot (secondary) right-of-way. Planned local streets shall provide a minimum of two 14 foot travel lanes (one each direction) and on-street parking opportunities. The required local street pattern shall allow the maximum flexibility to each arterial traffic flow. The pattern of local streets developed for the project area shall recognize the following:

- Streets shall be provided along property lines where possible.
- Local streets shall be planned to relieve traffic demand of arterials either by providing parallel routes or by local streets only feeding arterial streets.

The need for precise location, design, and the geometrics of local streets shall be submitted for approval at the time development is proposed. Such review shall be based upon the adopted Public Works Engineering/Design Standards.

### J. PRIVATE STREETS

Private streets may be desirable in new developments where they would enhance property identification, provide for control of access and where special overall design concepts may be involved. To ~~insure~~ensure that private streets are constructed in a manner which will not

create emergency access to hazards or future maintenance problems, the following shall be applied to approvals:

- Private streets shall be limited to cul-de-sacs and to minor local streets not carrying through traffic.
- Private streets shall be permitted only when access is provided from a public right-of-way.
- An irrevocable offer of dedication may be required of the developer at his expense prior to recordation.
- Private streets shall be constructed to the same standards as comparable local public streets in industrial areas to include a minimum of 44' of paving between curbs adopted to the City Engineering Design Standards. The construction of private streets will require a permit for encroachment onto a public right-of-way.
- All other provisions of Resolution 12220 dealing with private streets and drives shall also prevail.

Pedestrian access within Hunter Business Park shall be constructed in accordance with the following policies:

- Walkways within public street ~~right-of-ways~~ rights-of-way shall be City Engineering's standards and shall be located as provided for in the typical street cross sections (refer to Exhibit II-5: Roadway Sections).
- Internal walkways shall be separated from vehicular drives with a curb, berm or other appropriate means, and shall connect at safe and convenient points with sidewalks and street crossings with public rights-of-way.
- Pedestrian walkways shall directly link each building in the development project with parking areas, other buildings, and off-site areas.

### 1.3 Design Review

The design review procedures set forth in Title 19 ~~of the Riverside Municipal Code (Chapter 19.62)~~ shall apply to all commercial, office, and industrial properties in the Hunter Business Park Specific Plan Area.

## K DESIGN GUIDELINES

The Design Guidelines for Hunter Business Park and individual parcels will help ~~insure~~ ensure the implementation of a quality development and maintain the concept of a strong master-planned industrial community.

### 1.0 Architectural Concept

The architecture of Hunter Business Park shall possess an underlying compatibility among the various buildings through the use of harmonious materials, finishes, and colors while promoting individual expression and identity. The architectural style shall be in the





contemporary vocabulary. All buildings shall project simplicity while contrasting the solid mass elements with the elements of the glazing and openings. These comprise minimum architectural standards. Innovative design and the use of higher quality materials are encouraged.

## 2.0 Landscape Concept

The basic objective of the Landscape Concept is to create a strong sense of project identity throughout the Hunter Business Park Plan Area.

The intent of the Landscape Concept is to establish a park-like setting which creates not only a strong landscape framework for the buildings, but also provides a pleasant pedestrian circulation system along the major Gateway Streets. The Hunter Business Landscape Concept separates and defines three basic landscape zones:

1. Monument Entrances/Gateway Streets
2. Hierarchy of Streetscape Treatment
3. Individual Interior Parcels.

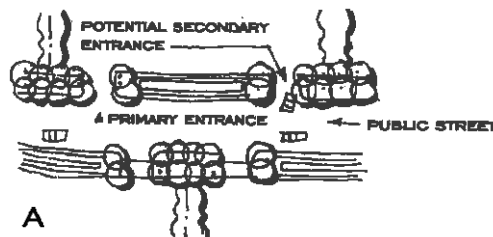
The selection of plant materials shall be made with water conservation and low maintenance characteristics as considerations. The aesthetic qualities of landscaping and plant material shall blend with the native plant material in the area especially at the edges of the project to Box Springs Mountain Regional Park.

Specific Landscape Design Criteria are presented in Section 4.0.

## 3.0 Site Planning Concepts

### 3.1 Entry Drives

1. Each parcel shall have at least one primary entry drive. The concept of this primary entrance point is to provide consistent formal plantings for trees of an evergreen variety. (See Diagram A).



2. Most parcels will have an opportunity for a secondary entry. Secondary entries are subject to approval by the City of Riverside Public Works Department and shall be

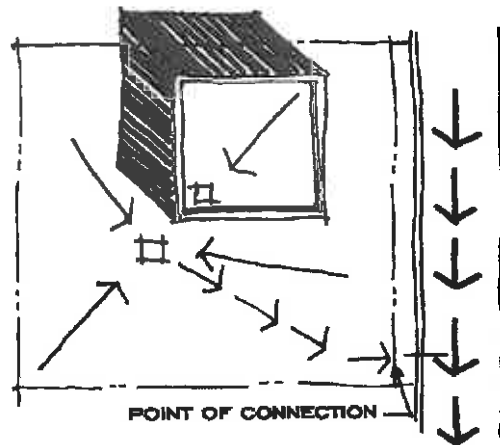
reviewed on an individual basis at the time of site design. This additional entrance shall include plantings of deciduous trees.

### 3.2 Parking

~~23.~~ Parking requirements for layouts, sizes, and ratios shall comply with ~~Chapter 24.21. 19.74 Title 19 Zoning of the Riverside Municipal Code.~~

### 3.3 Grading and Drainage

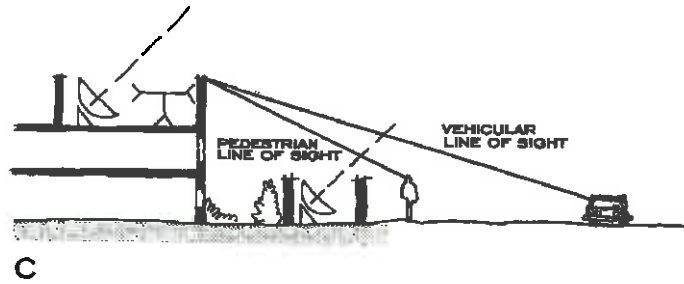
1. Drainage: Drainage of surface parking parcels and roof areas shall terminate at an underground storm drain system or approved street flow system. (See Diagram B).



B

### 3.4 Utilities and Communications Devices

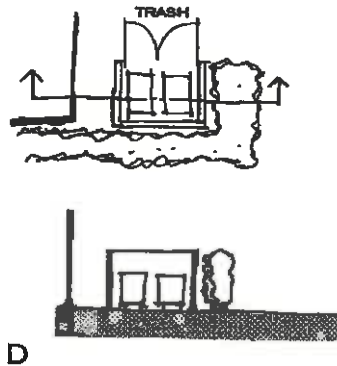
1. All new utilities, including those for rehabilitation building construction, shall be installed and maintained underground.
  2. Utilities shall be designed and installed to minimize disruption of off-site activity during construction.
  3. Temporary overhead power and telephone facilities shall be permitted only during construction.
  4. Any device for transmission or reception of communication signals shall be screened with compatible material and finishes as utilized in the building.
- A. Rooftop devices may not extend above the building's highest architectural element and shall not be visible within a horizontal line of sight from any major arterial or secondary collector. (See Diagram C.)



Ground-mounted devices shall be screened from view from adjacent streets and property with the same material and finishes as the adjacent buildings.

### 3.5 Trash and Transformers

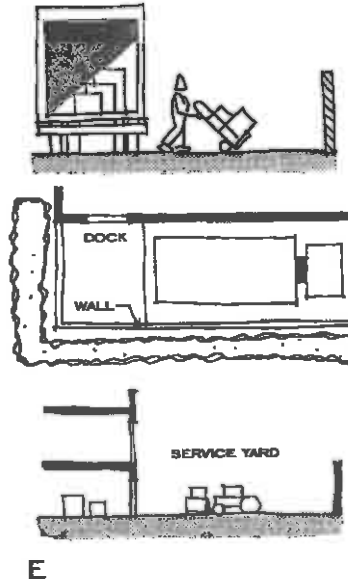
1. Outdoor trash areas shall be visually screened by a minimum ~~6-foot-high~~six-foot-high, noncombustible enclosure constructed of the same materials and finishes as the adjacent building. (See Diagram D).



2. Trash enclosures shall be designed and located so as not to be highly visible from adjacent streets and property.
3. Transformers and emergency generators, where required, shall be screened by walls or dense landscaping.
4. Above ground transformers and trash enclosures shall not be permitted within the "front" street side building setback. Transformers located in the "front" street side setback shall be underground.
5. Above-ground switching devices, installed as a part of the backbone utility system, shall be screened from view from adjacent streets by shrub elements as provided for in the streetscape design and shall not be permitted in the "front" street side setback.

### 3.6 Service and Delivery Areas

1. Loading docks and service delivery areas, where provided, shall be screened, recessed, and/or enclosed so as not to be visible from adjacent streets. (See Diagram E).

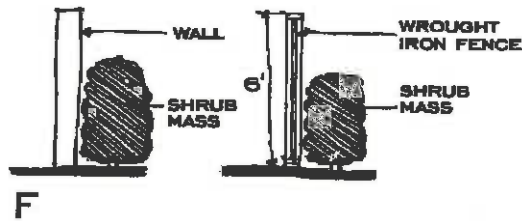


2. Service and delivery enclosures shall be a minimum of 8 feet high, noncombustible, and constructed of the same materials and finishes as the adjacent building.
3. The use of additional landscape materials to augment the enclosure walls shall be strongly encouraged.
4. No materials, supplies, or equipment, including company owned or operated trucks, shall be stored in any area on a site except inside a closed building or behind a visual barrier.

### 3.7 Wall/Fences

Walls/fences are to be utilized for either landscape design elements or to create visual barriers for screening purposes as outlined in 3.6 above.

1. Walls or fences shall be required as a means of screening when landscaping materials alone are insufficient. (See Diagram F).



2. Solid walls and fences required for screening, loading and outdoor storage areas shall be at least six (6) feet high. -
3. Walls separating properties within the Specific Plan and residentially zoned or used properties shall comply with the requirements of Title 19 of the Riverside Municipal Code.
- ~~25.~~ —A wall up to a maximum of twelve
- ~~26~~22. (12) feet high may be permitted ~~subject to the granting of a variance to~~ achieve screening requirements of this Chapter.
- ~~3.4.~~ Walls constructed within the required front setback areas shall not exceed three (3) feet in height.
- ~~4.5.~~ Walls shall be constructed of ~~masonry or concrete~~ materials consistent with, and complementary to, the building architecture.
- ~~5.6.~~ Landscaped block walls with suitable returns are required for screening purposes along the front yard setbacks. Maximum 8-foot high chain link fencing with adequate landscape screening shall be allowed along the side and rear property lines of interior lots where not visible from the public right-of-way. It is the applicant's responsibility to demonstrate that chain link fencing is not visible from the public right-of-way. Walls, fencing and landscape screening shall be subject to ~~Design Review Board~~ Community & Economic Development Department Director or designee approval.
- ~~6.7.~~ Walls or fences shall not be required between separate lots unless deemed necessary for security or screening purposes. Such walls located on property lines between lots shall not exceed six (6) feet in height.

### 3.8 Lighting

1. Lighting shall be used to provide illumination for security and safety of parking, loading, and access areas.
2. All lighting shall be shielded (cut off) to keep light spread within the site boundaries.
3. Pole light fixtures in parking areas shall not exceed thirty (30) feet in height. (See

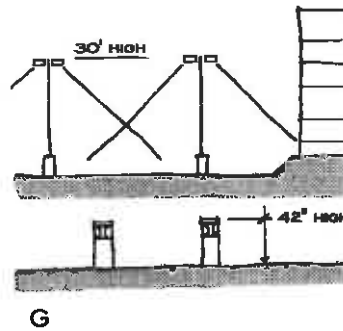


Diagram G).

4. Security light fixtures shall not project above the fascia or roofline of the building.

### 3.9 Rail Service

Rail service is an important asset of the industrial park. Therefore, uses which are dependent upon such rail service shall comply with the following standards:

1. Spur trackage shall not be permitted along any building frontage and shall be confined to the rear and sides of any buildings served.
2. At grade spur trackage, crossings shall be avoided, if possible.
3. All rail crossings and any spur construction must be approved by the affected railroad and the Public Utilities Commission.

### 4.0 Landscape Design Criteria

The landscape concept establishes three basic landscape zones:

- Monument Entry/Gateway Streets
- Streetscape Zone
- Individual Interior Parcel Zone

The following landscape design criteria define the overall planting, paving, (hardscape), lighting, and graphic concepts. Individual owners are allowed the flexibility to express their own design within the framework of these criteria.

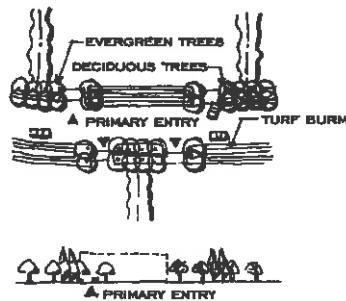
#### 4.1 Monument Entry/Gateway Streets Zone

The Entry Zone sets the image for Hunter Business Park and occurs at the major entrances off Columbia Avenue and Spruce Street. The landscape character shall consist primarily of turf, a backdrop of evergreen trees, and an accent element consisting of deciduous trees. Entry Zone design elements and materials are subject to Design Review Board review and approval.

## 4.2 Streetscape Zone

The Streetscape Zone is the primary landscape framework of the project and is intended to visually bind the project together. This zone consists of three components: Streetscape, Primary Driveway, and Secondary Driveway.

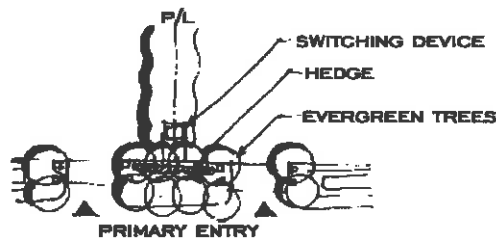
1. The Streetscape component consists of a continuous turf-bermed edge (along Columbia Avenue) and a consistent tree pattern of palms with alternating evergreen trees. It is intended that visual corridors are kept open at the central portion of each parcel to maximize the view of each building with concentration of formal tree statements identifying the entry points to each parcel near the side parcel lines. A continuous 6-foot walkway at the curb shall



H

be constructed on both sides of Iowa Avenue. (See Diagram H).

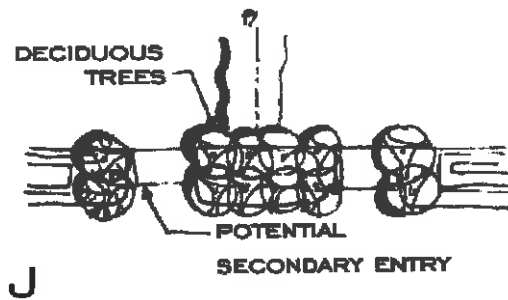
2. The Primary Driveway location shall be identified by a geometric tree pattern of an evergreen variety. The entry shall also incorporate the parcel address



I

number. (See Diagram I).

3. The Secondary Driveway location shall be identified by similar elements with the exception of the tree species, which shall be of a deciduous variety. (See Diagram J).



#### 4.3 On-Site Landscaping

On-Site Landscaping consists of three primary components:

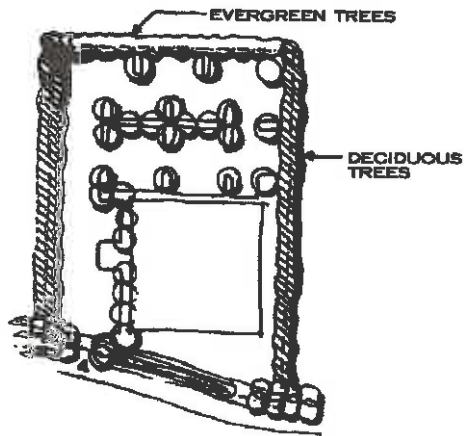
- Property Line Planting
- Parking Lot Planting
- Building Perimeter Planting

It is intended that individual owners have the flexibility to express their own landscape design concepts by being consistent with the overall landscape concept.

##### 1. Property line planting:

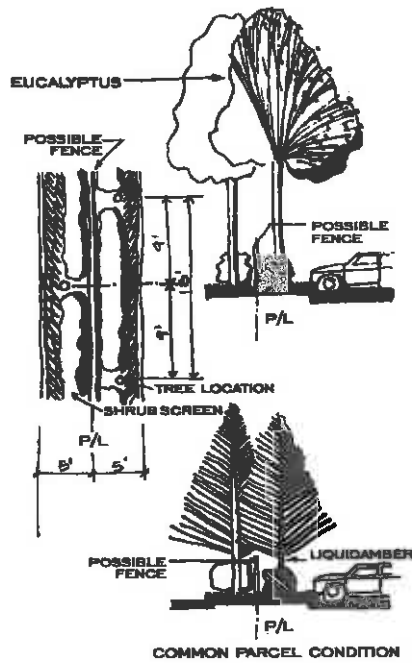
Property line trees are intended to work as "hedgerow" framing elements and shall be evergreen flowering varieties with an optimum, non-trimmed height for the shrubs of 6 to 7 feet. Shrubs shall be a maximum of 3 feet high within 25 feet of driveways for safe visibility. Special consideration shall be given to screening parked automobiles from adjacent properties. (See Diagram K).





## 2. Parking lot planting:

Parking lot trees shall shade the parking area, ~~and area and~~ break up the visual mass of the parking lot. These trees shall be located predominantly in islands and planters and shall have low ground cover throughout with shrub screening at the end bays of each island (See Diagram L).



L

### 3. Building Perimeter Planting:

Planting around building perimeters is intended to be colorful and more highly accented. Trees and shrubs shall have either colorful foliage or flowering characteristics except where restrictive areas require a more vertical species. Major building features shall utilize more intense plantings of colored ground cover and shrubs. Vines shall be encouraged at larger wall ~~massings~~massing and at loading and trash screen walls. (See Diagram M).

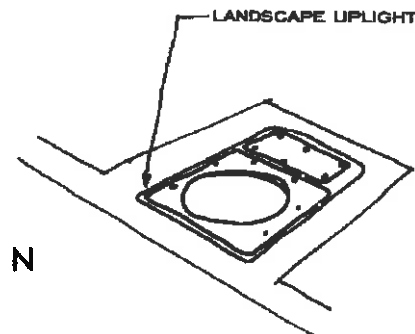


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### 4. Landscape Lighting:

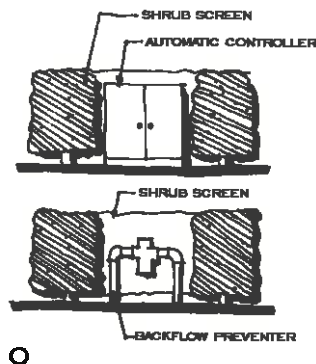
Landscape lighting, with no visible light source may be used for accenting the following conditions:

- Shrub masses
- Focal elements
- Trees (up-lights) (See Diagram N).



## 5. Irrigation

- All irrigation systems shall be fully automatic.
- Low-volume irrigation equipment shall be required for all planted areas within the individual sites.
- Irrigation water shall not overthrow onto walks, common areas, or onto any architectural walls.
- Utility cabinets and irrigation hardware shall be screened by evergreen shrubs. (See Diagram O).



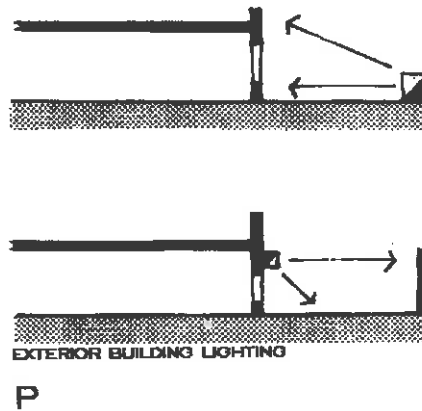
## 5.0 Architectural Design Criteria

### 5.1 Roofs

1. All rooftop equipment or devices including vents, louvers, hoods, and mechanical equipment shall be painted to match the building color.
2. Roof-mounted mechanical equipment shall not exceed above the highest architectural element or be mounted on any exterior building elevation. No such equipment shall be visible from the public right-of-way.
3. Any screening device shall be constructed of the same materials as the supporting building or closely matching materials.

## 5.2 Exterior Building Lighting

1. Exterior building lighting shall be used to reinforce the architectural design. Emphasis shall be placed on entries, landscaping elements, architectural features, etc.
2. Lighting shall be directed toward the building (wall wash), not adjacent

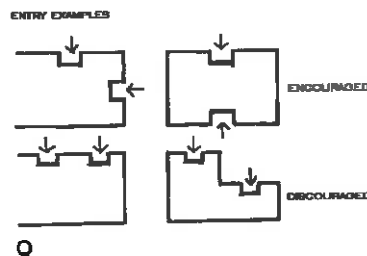


properties. (See Diagram P).

3. Service area lighting, if any shall be contained within the service area and shall be a 90 degree cutoff-type fixture.

## 5.3 Building Entries

1. A minimum number of entry points shall be encouraged for each building. (See Diagram Q).



**P20-0191 Sycamore Canyon Business Park Specific Plan Amdt.**

### 3.0 DEVELOPMENT STANDARDS AND CRITERIA

The following regulations are stipulated to implement the Sycamore Canyon Business Plan. As such, they will be utilized by the City of Riverside to ensure that future development proceeds in a consistent manner.

Development standards not specifically prescribed by the Specific Plan shall be governed by the appropriate provisions of Title 19- Zoning of the Riverside Municipal Code (Zoning Code) according to the underlying zoning designation of the site.

They have evolved from the analysis of local environmental opportunities and constraints as well as development needs. Based upon current City zoning standards set forth in the Business & Manufacturing Park (BMP) Zone, as described by Chapter 19.46-130 of the Zoning Ordinance Code, and the General Commercial Zone, as described by Chapter 19.36-110 of the Zoning Ordinance Code these standards have been modified where appropriate to achieve the goal and objectives of this Plan.

Standards have been organized into seven classifications, following a listing of typical permitted uses (Subsection 3.1), these include:

Subsection 3.2 - Lot Standards which define building site coverage, building heights, setbacks and lot sizes;

Subsection 3.3 - Parking Standards describing the basic design criteria for parking areas, a schedule of off-street parking requirements, and other related items;

Subsection 3.4 - Outdoor Storage and Loading Areas requirements design criteria;

Subsection 3.5 - Utilities and Lighting, requirements;

Subsection 3.6 - Sign Standards;

Subsection 3.7 - Screening of Mechanical Equipment criteria;

Subsection 3.8 - Trash Collection Areas requirements; and

Subsection 3.9 - Walls/Fence Standards defining locational and height requirements.

### 3.1 PERMITTED USES

This Specific Plan controls development by zoning properties to the BMP (Business & Manufacturing Park Zone), ~~MP-BP (Manufacturing Park with Business Park Combining Zone District~~ and C-3CG (General Commercial Zone), as defined in Chapters ~~19.46.020130~~ and ~~19.36.20110~~ and ~~19.44~~ of the City Code. The Plan further defines specific limitations and regulations as adjustments to this base zone.

Permitted uses are those land uses allowed in a given subarea (as shown in Exhibit 5).

"Industrial" subarea permitted uses are:

- "(1) Administrative or executive offices of a business or industrial establishment;"
- "(2) Manufacture, assembly, fabrication, warehousing, and wholesale distribution of goods, wares, merchandise, articles, or substances pursuant to the requirements of the Permitted Uses Table of Chapter 19.150 of the Zoning Code; provided, that goods, wares, merchandise, articles, or substances which are combustible, inflammable, explosive, or likely to create fire, radiation or explosive hazards to surrounding property may be stored and used in reasonable quantities as an incident to any permitted use only if such storage and use are allowed in the certificate of occupancy under such reasonable conditions as may be deemed necessary in the interests of public safety; ~~and further provided that any use prohibited in the "M-1" and 11M-211 zones shall also be prohibited in the "MP" zone;~~"
- "(3) Publishing and printing"
- "(4) Research offices and laboratories for the conduct of scientific research and theoretical studies and investigations, including the fabrication and testing of prototypes, and the performance of environmental tests, and related activities, by or under the supervision of professional scientists and highly trained specialists in the fields of physical, economic or social research;" .

The following uses shall be permitted in the "Industrial" subarea subject to a conditional use permit:

- "(A) Automobile service station, including indoor facilities for lubrication, battery and brake service, tire repair, minor adjustments and repair, but excluding painting, body work,

steam cleaning, major repairs, mechanical washing facilities, utility truck or trailer rental, and packaged ice sales, and in accordance with the provision of Section ~~19.28.020(35)~~410."

- "(B) Restaurant, cafe, or cafeteria".
- "(C) Any use appurtenant to and compatible with restricted industrial development which is consistent with the objectives of the planned industrial park concept."

~~Portions of the "Industrial" subarea are zoned "MP-BP" Manufacturing Park with a Business Park Combining Zone. The Combining Zone permits a limited range of retail commercial uses having a close association with, providing convenience to or which are compatible with the office, wholesale, warehousing and manufacturing uses permitted. For further details regarding the 'BP' Combining Zone see Chapter 19.44 of the Zoning Code.~~

Permitted uses for the "Industrial Support" subarea are limited by special constraints - the noise and accident potential impacts caused by flight operation at March Air Force Base directly to the south. High noise levels and accident risks require that development of this area be limited to low-density, low-concentration uses which will minimize the exposure to the public and employees to such impacts.

Accordingly, the Plan permits the following uses for "Industrial Support":

- Financial, insurance and real estate services;
- Professional services - typically, attorneys, architects, engineers, accountants;
- Automotive service stations- including minor repair of autos within enclosed buildings;
- Automotive rental agencies and related auto storage; and
- Business support uses involving clerical, employment, protective or similar services, including multi-copy and blueprint services.

Other compatible and related uses may be permitted subject to a conditional use permit where the following conditions apply:

- Use intensity no greater than one full time employee per 500 sq. ft. of building floor area, and
- No occupancy by the public of more than ten persons.



The "Industrial Support" subarea of the Plan is described in the March Air Force Base AICUZ study as "Compatible Use District 8" which involves a combination of "Accident Potential Zone II" and a noise level of Ldn 80-85. This combination of impacts requires that all uses include insulation of office areas and public reception areas to achieve a noise level reduction of 35 decibels.

The Plan permits the following uses for "Retail Business and Office" subarea:

- Retail shops, services and other similar commercial development. It also provides for low to moderate intensity office uses and for some visitor-serving commercial development. The typical development intensity for this category is a 0.25 Floor Area Ratio (FAR); the maximum development intensity is a 0.35 Floor Area Ratio (FAR).
- All uses permitted within the ~~C-3CG~~ General Commercial Zone per the ~~City's zoning ordinance~~ Zoning Code. For further information regarding the ~~C-3CG~~ zone see Chapter 19.36.020110 of the Zoning Code.
- "Open Space" is proposed for the portions of the Sycamore Canyon area with prevailing slope gradients of 15% or greater, (not isolated pockets over 15%); as well as areas with prime wildlife habitat and areas within manageable park boundaries. This term includes most of the "Natural Arroyo" as described in earlier plans for the Southeast Study Area.
- No metal curtain wall structures shall be permitted within any portion of the Plan Area.

### 3.2 LOT STANDARDS

The purpose of lot standards is to assure adequate development sites so that appropriate design measures (parking, ingress/ egress, building coverage) are feasible.

The minimum lot size shall be five acres; provided however, (1) that 30% of the area of each parcel of land of record existing as of the date this Specific Plan is adopted by the City Council may be subdivided to minimum one acre lots and developed in accordance with the BMP Development Standards defined in Chapter 19.46130; or 30% of the area of any combination of such parcels held under common ownership within the same Specific Plan designation, may, at the discretion of the Planning

Commission, be subdivided to minimum one acre lots; ~~Exempt from these lot size standards are those lots within the Motorfair Project of 136.9 vacant acres of land. Here, the minimum lot size shall be one acre, except the property fronting on Sycamore Canyon Boulevard shall be two acres;~~ (2) that a condominium- style industrial development may be permitted in such areas provided that such development contains one total acre or more; and (3) that such lots do not have frontage on thruways or collector streets.

Minimum lot widths shall be 300 feet for five acre and larger lots, and 140 feet for one acre lots.

### 3.2.1 SETBACK STANDARDS

The following setbacks shall be required within the Plan Area:

- Front Yards -

- A. For arterial streets and buildings over thirty feet in height: A front yard having an average depth of 50 feet but in no case less than 40 feet except as indicated below. The front 20 feet of such front yard shall be suitable landscaped with plant materials and adequately maintained. Notwithstanding any other provision of this Title to the contrary, pedestrian walks, vehicular access drives and railroad tracks which are perpendicular to the front property line in the front twenty feet, meter pits and utility manholes extending not more than six inches above finished grade, light fixtures and any recording instruments required by this Chapter shall be permitted in any front yard. Off-street parking, gate or guard houses, roofs or canopies covering unenclosed pedestrian walks and walls or fences not more than six feet in height shall be permitted in the rear 30 feet of any required front yard. A front yard having a minimum depth of forty feet shall be permitted when such yard is landscaped in its entirety, except that a driveway parallel to the front property line a maximum of 12 feet in width may be located within this landscaped front yard;
- B. For all other streets where the building is less than 30 feet in height: A front yard having a depth of 20 feet. The entirety of which shall be suitably landscaped with plant materials and adequately maintained. Notwithstanding any other provision of this Title to the contrary, pedestrian

walks, vehicular access drives and railroad tracks which are perpendicular to the front property line in the front twenty feet, meter pits and utility manholes extending not more than six inches above finished grade, light fixtures and any recording instruments required by this Chapter shall be permitted in any front yard.

- ~~Side and rear yard setbacks shall be required as set forth in the Title 19 of the Riverside Municipal Code Zoning Code based on the underlying zoning designation and the intended use or development of the site. Side Yard shall not be required, except that minimum side yards of fifty feet shall be required wherever a lot or parcel in the MP zone abuts a lot or parcel in any R zone. In the event a side yard abuts a street, such side yard shall meet all of the minimum requirements for a front yard and may be used in accordance with the front yard provisions pertaining to permitted uses;~~
- ~~Rear Yard shall not be required, except that a minimum rear yard of fifty feet shall be required wherever a lot or parcel in the MP zone abuts a lot or parcel in any R zone. In the event a rear yard abuts a street, such rear yard shall meet all of the minimum requirements for a front yard and may be used in accordance with the front yard provisions pertaining to permitted uses, with the exception of all lots adjacent to Sycamore Canyon Park which shall have a 20 foot landscaped building setback.~~

(Resolution No. 20347, 2003)

### 3.3 PARKING STANDARDS

Parking Standards are necessary to assure public safety and convenience, prevent congestion and increase site efficiency.

The following requirements for off-street parking and loading facilities apply to all development within the Plan.

The basic design requirements of all onsite parking areas shall conform to Chapter 19. ~~74580~~ of the City Code unless specifically amended by details of this Plan.

All required parking shall be provided onsite.

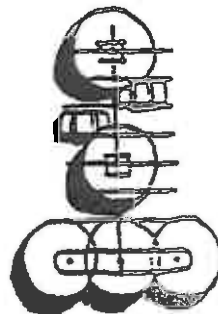
Setbacks for parking shall be 20 feet from the right-of-way.

Parking space shall be required as follows:

- Warehousing or building for storage:
  - A. One space per 1,000 sq. ft.;
- Manufacturing:
  - A. One space per 350 sq. ft.;
- Administrative/ Office:
  - A. One space per 250 sq. ft.;
- Mixed-use tenancy (storage/distribution/manufacturing with limited space devoted to exclusive office space) with less than 25% administrative/office use:
  - A. One space per 300 sq. ft.;

No required parking shall be provided within the required front setback area.

Landscaping within parking lots shall be reviewed and approved by the Design Review Board and conformance to standards of this Plan and the City Code. Such landscaping should include use of tree wells or planters at the end of parking bays.



Parking shall be screened from public view by means of landscaping, berms, and low masonry walls. However, vehicle display parking shall be exempt from this requirement in the retail portion of the Plan.

(Resolution No. 20437, 2003)

### 3.4 OUTDOOR STORAGE AND LOADING AREAS

All uses, except storage and unloading shall be conducted entirely within an enclosed building. Outdoor storage of material and equipment is permitted, provided the storage area is completely enclosed by walls and

the combined gross area of such area does not exceed ten (10) percent of the gross floor area of the building, no impinge upon any required parking or access ways.

No stored material may exceed the height of required walls.

All loading facilities and maneuvering areas must be on site with the use served.

Aisle width to loading docks shall be a minimum of fifty (50) feet exclusive of truck parking area.

All loading areas shall be screened from public view by landscaping or walls.

Loading facilities shall be prohibited in required front or side setback areas.

### **3.5 LIGHTING AND UTILITIES**

Lighting shall be used to provide illumination for security and safety of parking, loading, and access areas.

All lighting shall be shielded to keep light spread within the site boundaries.

Light fixtures in parking areas shall not exceed twenty-five (25) feet in height. Security light fixtures shall no project above the fascia or roofline of the building.

Streetlight fixtures shall not exceed thirty-five (35) feet in height.

All ground-mounted utility facilities such as transformers, back- flow preventors shall be adequately screened from public view.

All electrical lines less than 12KV and telephone lines within the Plan area shall be installed underground.

### **3.6 SIGN STANDARDS**

The Specific Plan recognizes that signs are not only a means of identifying businesses but are also an important design element. Sign provisions consistent with the Plan's objectives to ensure visually attractive, quality development is necessary.

The following standards apply to all signs within the Plan area:

All signs not expressly permitted are prohibited, including but not limited to the following:

- Roof signs;
- Flashing signs, except in time and temperature signs;
- Animated signs;
- Revolving signs;
- Vehicle signs, when used on property to identify a business;
- Portable signs;
- Off-site signs, except temporary subdivision directional signs;
- Signs on the public right-of-way, except where required by a governmental agency;
- Signs blocking doors or fire escapes;
- Light bulb strings and exposed tubing;
- Banners, flags, pennants and balloons; and Advertising structures.

Business identification wall-mounted and monument signs shall be permitted subject to the following provisions:

- Maximum number - one per street frontage, not to exceed two per lot for both wall mounted and monument signs;
- Maximum size - Ten (10) percent of the building face, not to exceed 150 square feet for wall signs and 24 square feet for monument signs;
- Maximum height - not to project above the roofline or parapet of the roof, and in no case be higher than 20 feet above finished grade for wall mount signs; and up to five feet for monument signs;

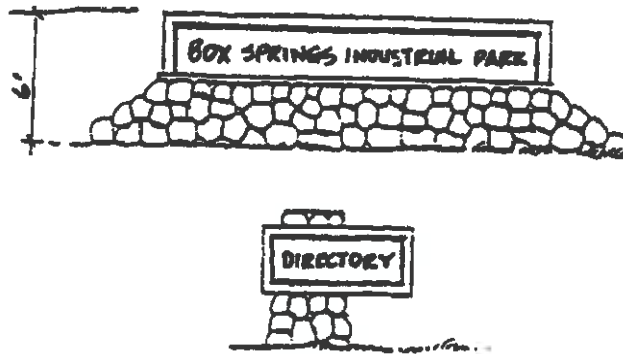
Each permitted monument sign shall be located in a planted landscaped area which is of a shape, design and size that will provide a compatible setting and ground definition to the sign. The planted landscaped area shall be maintained on a reasonable and regular basis.

Sign copy shall include minimal information only. The use of subordinate information such as a telephone number, lists of products, pictures of products, etc. is discouraged. The name of the business shall be the dominant message on the sign.

Monument signs shall be illuminated from an external light source. Wall signs may be illuminated either internally or externally.

All signs shall be designed to be architecturally compatible with the building.

- Signing for the Park is proposed as entry monuments at the major points of ingress. A wood sign with recessed lettering atop a native rock masonry base is suggested as shown.



- A similar design for Park directory signs is also shown.

### **3.7 DISPLAY MEDIANSDELETED.**

~~In the Motofair project (RBO land use designation) display medians within the right of way will be permitted provided that a covenant and agreement subject to approval by the City Attorney shall be recorded prior to adoption of zoning case R 48 878 that the adjacent property owners will maintain the display nodes and hold the City harmless if any traffic accidents occur involving the display nodes.~~

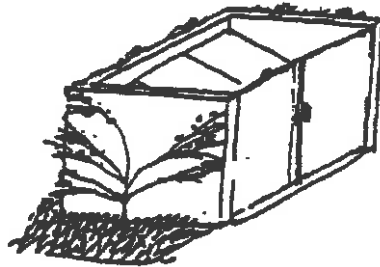
### **3.8 SCREENING OF MECHANICAL EQUIPMENT**

To the maximum extent possible, all utility and operational equipment located on the exterior of the building shall be screened from public view through the use of walls or landscaping.

### 3.9 TRASH COLLECTION AREAS

Storage and trash collection areas will be permitted in rear and interior side setback areas provided that trash containers are enclosed within a masonry wall with a metal gate.

Such container enclosures shall have a concrete base and have walls with a minimum height of six (6) feet.



### 3.10 WALLS/FENCE STANDARDS

Walls and fences will be required as a means of screening when landscaping materials alone would prove insufficient as determined by the Design Review Board.

Solid walls and fences required as a means of screening, loading and outdoor storage areas shall be at least six (6) feet high with a maximum of twelve (12) feet and will be constructed in accordance with the design criteria of this Plan.

Walls constructed within required front setback areas shall not exceed three (3) feet in height.

Walls shall be constructed of ~~masonry or concrete~~ materials consistent with, and complementary to, building architecture.

Walls or fences are not required between separate lots unless deemed necessary for security or screening purposes. Such walls located on property lines between lots shall not exceed six (6) feet in height.

Walls separating properties within the Specific Plan and residentially zoned or used properties shall comply with the requirements of Title 19 of the Riverside Municipal Code the Zoning Code.

Chain-link fencing shall be prohibited where it is visible from the public right-of-way.



### 3.11 RAIL SERVICE STANDARDS

Rail service is an important asset of the industrial park. Therefore, uses which are dependent upon such rail service shall comply with the following standards:

- Easement width for a lead line single rail track= 26 feet; for a double rail track= 41 feet;
- Rail lead track and nearby street elevation shall have approximately the same elevation;
- Minimum radius of curvature for track shall be 180 feet;
- Maximum permissible gradient along spur track shall be 1 1 to 2 %;
- Dock height shall be set between 4 ! to 5 feet above top of rail of the spur track;
- Spur trackage is not permitted along any building frontage and must be confined to the rear and sides of any buildings served; and
- At-grade crossings shall be avoided. All rail crossings and any spur construction must be approved by the AT&SF Railway Co. and the PUC.

**PAGE BREAK**



4.2

RESOLUTION NO. 2020-01

**A RESOLUTION OF THE RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION (ALUC) EXTENDING THE AUTHORIZATION OF THE ALUC DIRECTOR TO TAKE ACTION ON LEGISLATIVE ITEMS IN AIRPORT COMPATIBILITY ZONE E OF THE MARCH AIR RESERVE BASE/INLAND PORT AIRPORT INFLUENCE AREA**

WHEREAS, on August 13, 2015, the Riverside County Airport Land Use Commission (ALUC) adopted ALUC Resolution No. 2015-01 authorizing the ALUC Director to review general plan amendments, specific plan amendments, zone changes, and variance applications submitted to the ALUC for land use development located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area pursuant to Public Utilities Code section 21676 and to render a consistency determination as to any such application that is not inconsistent nor has the possibility to be inconsistent with the land use planning guidelines contained therein; and,

WHEREAS, this authorization was granted for a one-year period ending August 31, 2016; and,

WHEREAS, by the adoption of Resolution No. 2016-02, the ALUC extended the ALUC Director's authority granted pursuant to Resolution No. 2015-01 for two years to expire on August 31, 2018; and on adoption of Resolution 2018-02 the ALUC extended the ALUC Director's authority for two years to expire on August 31, 2020, unless renewed; and,

WHEREAS, the ALUC has decided that this authorization should be extended for a period of five additional years; therefore,

BE IT RESOLVED, FOUND and DETERMINED that the ALUC, by the adoption of this resolution, hereby extends the ALUC Director's authority granted pursuant to Resolution No. 2015-01 for an additional five years to expire on August 31, 2025, unless renewed.

BE IT FURTHER RESOLVED, FOUND and DETERMINED that the ALUC will continue to be advised of all proposals reviewed by the ALUC Director.

NOW, THEREFORE, the ALUC adopts by the following vote:

AYES:

NOES:

ABSENT:

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**Chairman  
Riverside County ALUC**

**WITNESS, my hand this 13th day of August, 2020.**

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**Barbara Santos  
ALUC Secretary**



**AIRPORT LAND USE COMMISSION MEETING  
MINUTES  
JULY 9, 2020**

**DRAFT**

7-16-20

**COMMISSIONERS PRESENT:** Russell Betts, Steven Stewart, Arthur Butler, John Lyon, Steve Manos, Richard Stewart, Gary Youmans (Present via Zoom, pursuant to California Executive Order N-29-20).

**COMMISSIONERS ABSENT:** None

**2.0 PUBLIC HEARING: CONTINUED ITEMS**

2.1 Staff report recommended: **CONSISTENT**

Staff recommended at hearing: **CONSISTENT**

ALUC Commission Action: **CONSISTENT (Vote 7-0)**

**Motion: Richard Stewart**  
**Second: Steven Stewart**

**ZAP1409MA20 – Vanagan Holdings, Inc. (Representative: JM Civil Engineering)** – County of Riverside Case No. PPT190029 (Plot Plan). A proposal to construct a 77,492 square foot industrial warehouse building with mezzanine (in two phases) on 3.99 acres, located westerly of Patterson Avenue, southerly of Cajalco Road, easterly of Seaton Avenue, and northerly of Rider Street (Airport Compatibility Zone C2 of the March Air Reserve Base/Inland Port Airport Influence Area). Continued from May 14, and June 11, 2020. Staff Planner: Paul Rull at (951) 955-6893, or e-mail at [prull@rivco.org](mailto:prull@rivco.org)

2.2 Staff report recommended: **CONTINUE to 8-13-20**

Staff recommended at hearing: **CONTINUE to 8-13-20**, with the applicants consent.

ALUC Commission Action: **CONTINUED to 8-13-20**, with the applicants consent. **(Vote 7-0)**

**Motion: Richard Stewart**  
**Second: Steven Stewart**

**ZAP1405MA20 – Riverside Inland Development, LLC/Hillwood Investment Properties (Representative: Kathy Hoffer)** – March Joint Powers Authority Case Nos. PP20-02 (Plot Plan), TPM20-02 (Tentative Parcel Map No. 37220). The applicant proposes to construct a 2,022,364 square foot industrial warehouse building (maximum 54 feet in height) with mezzanines on 142.5 acres located easterly of Interstate 215, southerly of March Field Air Museum and the easterly terminus of Van Buren Boulevard, northerly of Nandina Avenue, and westerly of the runways at March Air Reserve Base. The applicant also proposes to change the Veterans Industrial Park 215 Specific Plan (SP16-02), updating Section 4.3 Landscaping Guidelines to reflect ALUC wildlife hazard goals and policies. The applicant also proposes to merge the project’s five parcels into one parcel. (A previous proposal to establish two industrial buildings (maximum 48 feet in height) totaling 2,185,618 square feet on this site had been found consistent by the ALUC, but no action was taken by the March Joint Powers Authority Commission) (Airport Compatibility Zone B2 of the March Air Reserve Base/Inland Port Airport Influence Area). Continued from May 14, and June 11, 2020. Staff Planner: Paul Rull at (951) 955-6893, or e-mail at [prull@rivco.org](mailto:prull@rivco.org)

**VIDEO:**

A video recording of the entire proceedings is available on the ALUC website at [www.rcaluc.org](http://www.rcaluc.org). If you have any questions please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at [basantos@rivco.org](mailto:basantos@rivco.org)

**AIRPORT LAND USE COMMISSION MEETING  
MINUTES  
JULY 9, 2020**

- 2.3 Staff report recommended: **ZAP1084PS20 – Terra-Gen Development/Coachella Wind Holdings, LLC (Representative: Armand Anselmo) – Related Case: City of Palm Springs Plan Check Case No. 2020-1140.e.**  
**CONSISTENT**
- Staff recommended at hearing: **CONSISTENT**
- ALUC Commission Action: **CONSISTENT (Vote 7-0)**
- Motion: Gary Youmans**  
**Second: Arthur Butler**
- The applicant proposes to construct one permanent meteorological tower 311 feet in height within an 860-acre wind turbine development approved through Conditional Use Permit No. 5.1429 located southerly of Interstate 10, westerly of Indian Canyon Drive, and northerly of Highway 111. The applicant previously received approval to decommission and remove approximately 363 commercial wind turbines and install 20 new commercial wind turbines with a maximum height of 499 feet on this site. That project was reviewed by the Airport Land Use Commission as ZAP1070PS18. However, the specific location of the meteorological tower was not yet determined at that time. This application is submitted pursuant to Condition No. 5 of ZAP1070PS18 requiring any proposal for new structures taller than 200 feet ground level to be submitted to ALUC for review. (Not located within an Airport Compatibility Zone). Continued from June 11, 2020. Staff Planner: John Guerin at (951) 955-0982, or e-mail at [jguerin@rivco.org](mailto:jguerin@rivco.org)

**3.0 PUBLIC HEARING: NEW CASES**

- 3.1 Staff report recommended: **ZAP1390MA19 – PR Partners, LLC (Representative: Mike Naggar & Associates)– City of Perris Case Nos. PLN19-00012 (Specific Plan Amendment), PLN19-05287 (Zone Change), DPR19-00012 (Development Plan Review).**  
**CONTINUE to 8-13-20**
- Staff recommended at hearing: **CONSISTENT**, subject to the conditions included herein, and Air Force concurrence letter submitted at the meeting
- ALUC Commission Action: **CONSISTENT**, subject to the conditions included herein, and Air Force concurrence letter submitted at the meeting  
**(Vote 7-0)**
- Motion: Steve Manos**  
**Second: Gary Youmans**
- A proposal to construct a 347,919 square foot industrial e-commerce and warehouse building on 16.1 acres located on the southwest corner of Perris Boulevard and Ramona Expressway. Also proposed is an amendment to the Perris Valley Commerce Center Specific Plan, and a proposal to change the site's zoning from Commercial to Light Industrial (Airport Compatibility Zones B1-APZ-II and C1 of the March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at [prull@rivco.org](mailto:prull@rivco.org)

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**AIRPORT LAND USE COMMISSION MEETING  
MINUTES  
JULY 9, 2020**

- 3.2 Staff report recommended: **CONTINUE to 8-13-20**
- Staff recommended at hearing: **CONSISTENT**, subject to the conditions included herein, and Air Force concurrence letter submitted at the meeting
- ALUC Commission Action: **CONSISTENT**, subject to the conditions included herein, and Air Force concurrence letter submitted at the meeting  
**(Vote 7-0)**
- Motion: Richard Stewart**  
**Second: Arthur Butler**
- ZAP1425MA20 – Rockefeller Group (Representative: EPD Solutions) – County of Riverside Case No. BNR2000056 (Building Permit).** A proposal to construct 210,000 square feet of rooftop solar panels on a 345,006 square foot industrial manufacturing building on 16.86 acres, located easterly of Harvill Avenue, westerly of Interstate 215 Freeway, southerly of Orange Avenue, and northerly of Daytona Cove (The previous proposal to construct the 345,006 square foot industrial manufacturing building at this site had been found consistent by the ALUC) (Airport Compatibility Zone C2 of the March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at [prull@rivco.org](mailto:prull@rivco.org)

- 3.3 Staff report recommended: **CONSISTENT**
- Staff recommended at hearing: **CONSISTENT**
- ALUC Commission Action: **CONSISTENT (Vote 7-0)**
- Motion: Richard Stewart**  
**Second: Arthur Butler**
- ZAP1419MA20 – MS Van Buren II, LLC (Representative: SDH & Associates, Rob Van Zanten)** – March Joint Powers Authority Case No. TPM20-03 (Tentative Parcel Map No. 37091). A proposal to divide 13.60 acres located on the northwest corner of Van Buren Boulevard and Meridian Parkway into four commercial parcels. (The previous proposal to construct 11 shell buildings totaling 160,608 square feet (as well as a Specific Plan Amendment) on this site had been found consistent by the ALUC (Airport Compatibility Zone C1 of the March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at [prull@rivco.org](mailto:prull@rivco.org)

4.0 **ADMINISTRATIVE ITEMS**

4.1 **Director’s Approvals** – Information Only

4.2 **Detention Basins and Wildlife Hazards**

Simon Housman, ALUC Director informed the Commission that per their suggestion staff reached out to the Riverside County Vector Control for approval to add their name to the storm water basin signs. Staff has not heard back from the Vector Control at this time. In the event that they are not willing to have their name on the signs staff will reach out to the Code Enforcement and other appropriate agencies.

5.0 **APPROVAL OF MINUTES**

Vice Chair Steven Stewart motioned to approve the June 11, 2020 minutes. Seconded by Commissioner Youmans. (Vote 7-0)

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**AIRPORT LAND USE COMMISSION MEETING  
MINUTES  
JULY 9, 2020**

**6.0 ORAL COMMUNICATION ON ANY MATTER NOT ON THE AGENDA**

Simon Housman, ALUC Director informed the Commission that the Office of Economic Assistance has approved the grant for the March Air Reserve Base Compatibility Use Study (CUS).

**7.0 COMMISSIONER'S COMMENTS**

Commissioner Richard Stewart requested an update from the ALUC Director regarding the current status of ALUC's budget. Simon Housman, ALUC Director replied that he is unaware of any changes to ALUC's budget for what is now the current fiscal year beginning July 1. If there are any budget adjustments in the near future Mr. Housman will notify the Commission. Russell Betts, Chair commented his concerns regarding the cumulative impact of multiple solar projects are not currently analyzed in the solar glare studies. Simon Housman, ALUC Director replied that staff will be addressing appropriate ways to analyze cumulative impacts on the solar glare issues.

**8.0 ADJOURNMENT**

Chairman Betts adjourned the meeting at 10:21 a.m.

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**VIDEO:**

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