

AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY AGENDA

Riverside County Administrative Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside, California

Thursday 9:30 A.M., October 11, 2018

CHAIR

Steve Manos
Lake Elsinore

VICE CHAIR

Russell Betts
Desert Hot Springs

COMMISSIONERS

Arthur Butler
Riverside

John Lyon
Riverside

Steven Stewart
Palm Springs

Richard Stewart
Moreno Valley

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Temecula

STAFF

Director
Simon A. Housman

John Guerin
Paul Rull
Barbara Santos

County Administrative Center
4080 Lemon St, 14th Floor
Riverside, CA 92501
(951) 955-5132

www.rcaluc.org

NOTE: If you wish to speak, please complete a "SPEAKER IDENTIFICATION FORM" and give it to the Secretary. The purpose of the public hearing is to allow interested parties to express their concerns. Comments shall be limited to 5 minutes and to matters relevant to the item under consideration. Please do not repeat information already given. If you have no additional information, but wish to be on record, simply give your name and address and state that you agree with the previous speaker(s). Also please be aware that the indicated staff recommendation shown below may differ from that presented to the Commission during the public hearing.

Non-exempt materials related to an item on this agenda submitted to the Airport Land Use Commission or its staff after distribution of the agenda packet are available for public inspection in the Airport Land Use Commission's office located at 4080 Lemon Street, 14th Floor, Riverside, CA 92501 during normal business hours.

Live Streaming of the meeting will be available during the meeting on our website at www.rcaluc.org.

In compliance with the Americans with Disabilities Act, if any accommodations are needed, please contact Barbara Santos at (951) 955-5132 or E-mail at basantos@rivco.org. Request should be made at least 48 hours or as soon as possible prior to the scheduled meeting.

1.0 INTRODUCTIONS

1.1 CALL TO ORDER

1.2 SALUTE TO FLAG

1.3 ROLL CALL

2.0 PUBLIC HEARING: CONTINUED ITEMS

PALM SPRINGS INTERNATIONAL AIRPORT

- 2.1 ZAP1068PS18 – Painted Hills Wind, LLC (Representative: Robert Skaggs) – Riverside County Planning Case Nos. WCS180001 (Commercial WECS Permit) and VAR180003 (Variance). The applicant proposes to decommission and remove approximately 291 existing commercial wind turbines and install 14 new commercial wind turbines with a maximum height of 499 feet with a per turbine energy generating capacity between 2.0 megawatts (MW) and 4.2 MW on 600 acres located northerly of Avenue 16, easterly of Whitewater Canyon Road, westerly of Windhaven Road at the terminus of Painted Hills Road. Also proposed are associated equipment such as a laydown yard, construction of new temporary and permanent internal roads, and new 12-kilovolt (kV) underground/overhead electrical collection lines. A variance is also requested proposing reductions in safety, wind access, and scenic setbacks. (Not located within an Airport Compatibility Zone). Continued from 9-13-18. Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

Staff Recommendation: **CONDITIONALLY CONSISTENT**

BERMUDA DUNES AIRPORT

- 2.2 ZAP1075BD18 – City of Indio (Leila Namvar, Development Services Department). A proposal by the City of Indio to adopt a new General Plan 2040 to guide the future development of the City that focuses on revitalizing and connecting neighborhoods, establishing a human-scale network of complete streets and community open spaces, and enhancing community health and wellness. The General Plan includes the following elements/chapters: Vision and General Plan Strategies, Land Use and Urban Design, Mobility, Economic Development, Health and Equity, Parks, Recreation, and Open Space, Conservation, Infrastructure and Public Facilities, Safety, Noise, and Implementation. (The Housing Element is also part of the General Plan, but is not proposed for change as part of this effort.) The City includes land within all Compatibility Zones of the Bermuda Dunes Airport Influence Area. Continued from 9-13-18. Staff Planner: John Guerin at (951) 955-0982, or e-mail at jguerin@rivco.org

Staff Recommendation: INCONSISTENT

3.0 PUBLIC HEARING: NEW ITEMS**PALM SPRINGS INTERNATIONAL AIRPORT**

- 3.1 ZAP1069PS18 – J.J.L. Ibarra (Representative: Wendell Veith, Architect) – City of Cathedral City Planning Case No. CUP 18-009 (Conditional Use Permit). The applicant proposes to construct a 2,602 square foot automobile repair shop building on a 0.2-acre parcel located at the northwest corner of Sarah Street and Agua Caliente Trail (Airport Compatibility Zones C and D of the Palm Springs International Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org.

Staff Recommendation: CONSISTENT

JACQUELINE COCHRAN REGIONAL AIRPORT

- 3.2 ZAP1044TH18 – Peter Solomon (Representative: The Altum Group) – City of Coachella Planning Case Nos. CZ18-09 (Change of Zone), CUP307 (Conditional Use Permit). The applicant proposes to construct five new industrial buildings and expand and change the use of one existing building on 10.0 acres located on the northeast corner of Avenue 54 and Polk Street. The applicant has identified tenants who plan to utilize the existing building and building 1 for cannabis cultivation operations. Tenants have not been identified for buildings 2 through 5. The applicant also proposes to change the site's zoning from Heavy Industrial (M-H) to Manufacturing Services – Industrial Park Overlay Zone (MS-IP). No dispensaries are proposed. (Airport Compatibility Zone D of the Jacqueline Cochran Regional Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

Staff Recommendation: CONSISTENT

BANNING MUNICIPAL AIRPORT

- 3.3 ZAP1031BA18 – Diana and John Hanna – City of Banning Case Nos. GPA18-2504 (General Plan Amendment), ZC18-3503 (Zone Change), and DR18-7011 (Design Review). DR18-7011 is a proposal to develop a 96-unit apartment complex with clubhouse on 7.08 – 7.34 acres (Assessor's Parcel Numbers 534-283-011 and 534-283-014) located westerly of Hathaway Street and southerly of Hoffer Street (northerly of an existing apartment complex). GPA18-2504 proposes to amend the designation of the project site on the City's Land Use Map from Low Density Residential to High Density Residential. ZC18-3503 proposes to amend the zoning of the site from Low Density Residential to High Density Residential. (Airport Compatibility Zone D of the Banning Municipal Airport Influence Area). Staff Planner: J. Guerin at (951) 955-0982, or e-mail at jguerin@rivco.org

Staff Recommendation: CONSISTENT(GPA, ZC); CONTINUE to 11-8-18 (Design Review)

MARCH AIR RESERVE BASE

- 3.4 ZAP1326MA18 – Stratford Ranch Investors, LLC (Representative: Mission Pacific Land Company, Jason Keller) – City of Perris Case Nos. 16-05217 (General Plan Amendment), 16-05218 (Zone Change), 16-05216 (Tentative Tract Map No. 36647). The applicant proposes to subdivide 24.06 gross acres located westerly of Evans Road, northerly of Ramona Expressway, and easterly of Oleander Avenue into 90 single family residential lots, amend the site's General Plan land use designation from Specific Plan (SP) to Single Family Residential R-6,000, and change its zoning from Single Family Residential R-10,000 to Single Family Residential R-6,000. (Airport Compatibility Zone D of the March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

Staff Recommendation: CONSISTENT

- 3.5 ZAP1327MA18 – Mobilitie, LLC (Representative: Robert Schultz) – March Joint Powers Authority Case No. CUP18-01 (Conditional Use Permit). A proposal to remove an existing 26 foot tall street light pole and replace it with a new 33.3 foot tall wireless facility street light pole (and associated equipment) located on the westerly side of Innovation Drive, within the street right-of-way southerly of Cactus Avenue. (Airport Compatibility Zone B2 of the March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

Staff Recommendation: CONDITIONALLY CONSISTENT

- 3.6 ZAP1328MA18 – Truck Terminal Properties (Representative: Joseph E. Bonadiman & Associates, Inc.) – March Joint Powers Authority Case Nos. Change of Zone 17-01, Plot Plan 17-07. The applicant proposes to establish a truck parking terminal consisting of 258 truck trailer parking spaces (and 3 regular vehicle parking spaces) totaling 11.2 acres of paved parking area, a 450 square foot security office, and 0.36 acres of basin area, on a 13.68 acre site located westerly of Heacock Street, northerly of the westerly extension of San Michele Road, and easterly of the March Air Reserve Base. The applicant also proposes a change of zone for the site to Industrial. (Airport Compatibility Zone B2 of the March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

Staff Recommendation: CONSISTENT(Change of Zone); CONDITIONALLY CONSISTENT (Plot Plan)

MARCH AIR RESERVE BASE

- 3.7 ZAP1274MA17 – Riverside Inland Development, LLC/Hillwood Investment Prop. (Representative: Kathy Hoffer) – March Joint Powers Authority Case Nos. General Plan Amendment No. 16-01, Specific Plan 16-02, Plot Plan 16-06, Parcel Map 16-02 (Tentative Parcel Map No. 37220). The applicant proposes to construct two industrial buildings totaling 2,185,618 square feet on 142.5 acres located easterly of Interstate 215, southerly of March Field Air Museum and the easterly terminus of Van Buren Boulevard, northerly of Nandina Avenue, and westerly of March Air Reserve Base. The applicant also proposes to amend the March Joint Powers Authority General Plan to include general warehousing/logistics uses as allowable land uses on lands designated as “Aviation” (AV), to amend the site’s designation from “AV” to “AV (SP-8)”, and to update the Building Capacity table in the Land Use Element. Specific Plan No. 16-02 is a proposal for a new Specific Plan (SP-8) that provides goals, policies, programs, land uses, development standards, and design guidelines for development on this site. Tentative Parcel Map No. 37220 proposes to divide the site into two parcels (one for each building). (Airport Compatibility Zone B2 of the March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

Staff Recommendation: INCONSISTENT

4.0 **ADMINISTRATIVE ITEMS**

4.1 Director’s Approvals

4.2 ALUC Director’s Report: The Path Forward Following the Release of the 2018 Air Installation Compatible Use Zones Report (2018 AICUZ) for March Air Reserve Base/Inland Port Airport

4.3 Nighttime Aircraft Operations at the March Inland Port Airport

5.0 **APPROVAL OF MINUTES**

September 13, 2018

6.0 **ORAL COMMUNICATION ON ANY MATTER NOT ON THE AGENDA**

7.0 **COMMISSIONER’S COMMENTS**

COUNTY OF RIVERSIDE AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM: 2.1 ~~3.2~~

HEARING DATE: **October 11** ~~September 13~~, 2018

CASE NUMBER: ZAP1068PS18 – Painted Hills Wind, LLC (Representative: Robert Skaggs)

APPROVING JURISDICTION: County of Riverside

JURISDICTION CASE NO.: WCS180001 (Commercial WECS Permit), VAR180003 (Variance)

MAJOR ISSUES: The project is currently being reviewed by the Federal Aviation Administration Obstruction Evaluation Service for impacts to air navigation. There are significant concerns regarding the extreme height of the proposed wind turbines, as well as electromagnetic interference and/or interference to the airport approach/departure radar surveillance system. The project is being recommended *to the Commission as conditionally consistent, subject to the review and conditions by the FAAOES. In the event that the FAA review determines that the project is a hazard to air navigation, the project will be brought back to the Commission for a revised determination that will incorporate the completed FAA review.* ~~for a continuance until such time that the FAA review is completed.~~

RECOMMENDATION: *Staff recommends that the Commission find the proposed Commercial Wind Energy Conversion Systems (WECS) Permit and Variance CONDITIONALLY CONSISTENT, subject to the conditions included herein, and such additional conditions as may be required by the Federal Aviation Administration Obstruction Evaluation Service. Staff recommends ~~CONTINUANCE of the project to the Commission's October 11 meeting, unless Determinations of No Hazard to Air Navigation are issued by September 13.~~*

PROJECT DESCRIPTION: The applicant proposes to decommission and remove approximately 291 existing commercial wind turbines and install 14 new commercial wind turbines with a maximum height of 499 feet with a per turbine energy generating capacity between 2.0 megawatts (MW) and 4.2 MW on 600 acres. Wind turbine heights are measured **from the base of turbine pole (bolted down to the foundation)** at top of blade in the “twelve o’clock position”. **The applicant has represented that the combined height of the turbine and foundation will be less than 500 feet above ground level (AGL).**

Also proposed are associated equipment such as ~~two meteorological towers~~, laydown yard, construction of new temporary and permanent internal roads, and new 12-kilovolt (kV) underground/overhead electrical collection lines. A variance is also requested proposing reductions in safety, wind access, and scenic setbacks.

PROJECT LOCATION: The project is located northerly of Avenue 16, easterly of Whitewater

Canyon Road, westerly of Windhaven Road at the terminus of Painted Hills Road.

The project site is not located within an existing Airport Influence Area, as it lies over 44,000 feet from the northwesterly terminus of the primary runway (Runway 13R-31L) at Palm Springs International Airport, but the project comes before the Airport Land Use Commission because of its inclusion of structures exceeding 200 feet in height.

BACKGROUND: As stated in Section 1.5.3.c of the Countywide Policies of the Riverside County Airport Land Use Compatibility Plan, “any proposal for construction or alteration of a structure (including antennas) taller than 200 feet above the ground level at the site” requires referral to the Airport Land Use Commission for a determination of consistency with the Commission’s Plan prior to approval by the local jurisdiction. Such facilities also require notification to the FAA pursuant to Part 77, Paragraph 77.13. The Riverside County Airport Land Use Compatibility Plan (RCALUCP) Policy Document, adopted on October 14, 2004, does not articulate specific procedures or criteria to evaluate such facilities by the Airport Land Use Commission.

As such, the determination by FAA’s Obstruction Evaluation Service (through the Form 7460-1 process) is pivotal in providing a basis for the ALUC’s decision on such a facility. The applicant has submitted Form 7460-1, and FAA OES has assigned Aeronautical Study Nos. 2018-WTW-8741-OE through 2018-WTW-8754-OE, to this project, which is recognized as a “Work in Progress” as of the date of this staff report.

The project description proposes two meteorological (MET) towers that the applicant has indicated will be 309 feet tall. However, they are not included in the current application the FAA OES is reviewing for the wind turbines, and the applicant has stated that specific locations for these MET towers have not yet been determined. Therefore, when ALUC renders its determination, it will be based on the project scope minus these MET towers as they have not been reviewed by the FAA. Consequently, ALUC’s recommended conditions for this project will specify that any proposed MET towers exceeding 200 feet in height will require a separate ALUC application, review, and determination, along with a complete FAA determination of no hazard letter.

CONDITIONS:

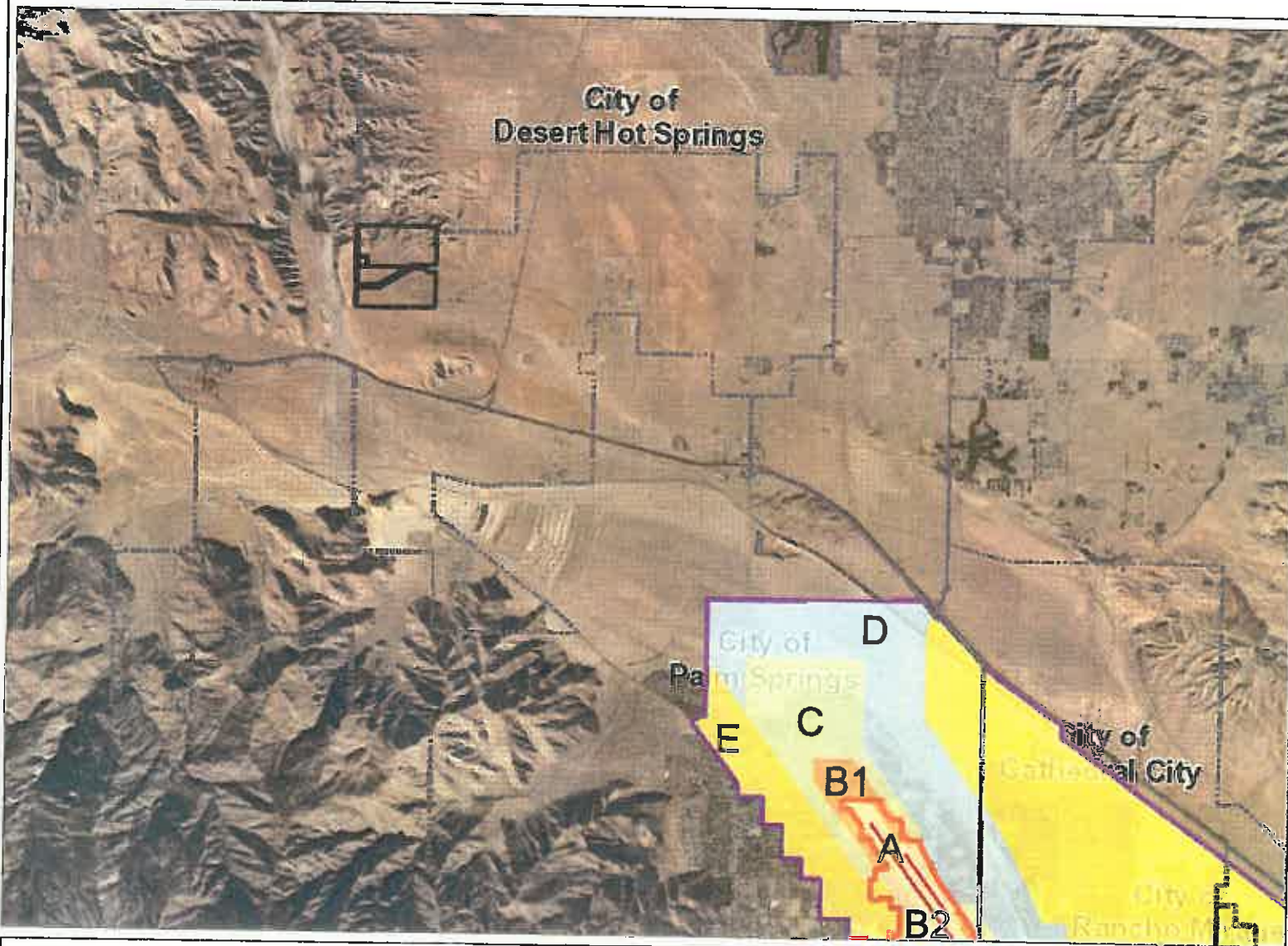
1. **The proposed WECS shall not generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.**
2. **Rotor blades shall utilize a flat or matte (non-glossy) finish so as to minimize the reflection of sunlight towards an aircraft engaged in an initial straight climb during takeoff or towards an aircraft engaged in a straight final approach toward a landing at an airport.**
3. **The WECS and any accessory uses shall not generate smoke or water vapor and shall be designed so as not to attract large concentrations of birds.**

4. **The combined height of each WECS and its foundation will be less than 500 feet above ground level (AGL).**
5. **This project has been evaluated by Airport Land Use Commission (ALUC) and the Federal Aviation Administration (FAA) for the 14 wind turbines only. Any increase in number, height, or change in location of the turbines, or any proposal for new structures taller than 200 feet from ground level, will require review by the ALUC and FAA.**

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones
- ▨ OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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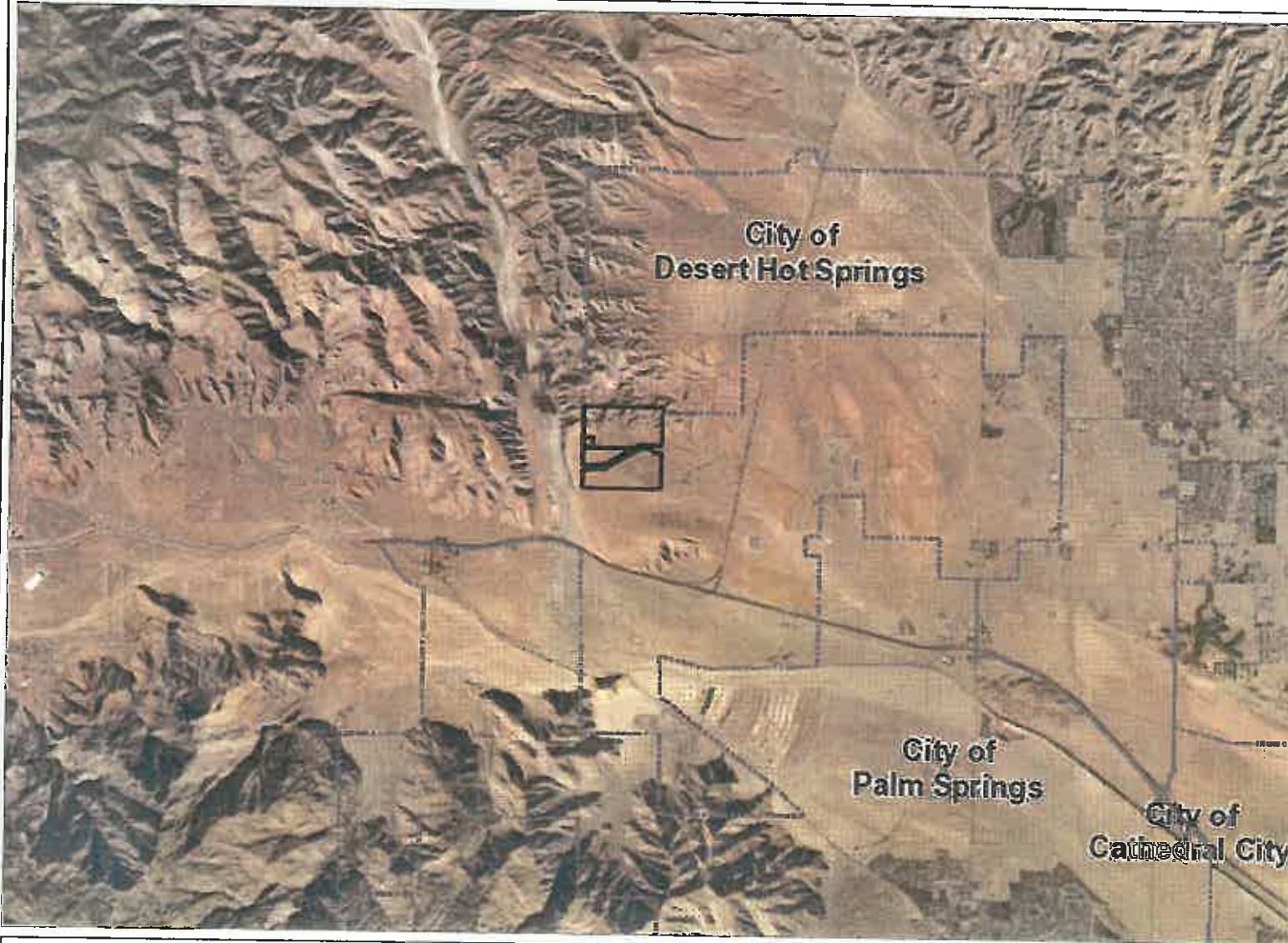


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Notes

Map My County Map



Legend

- City Areas
- World Street Map



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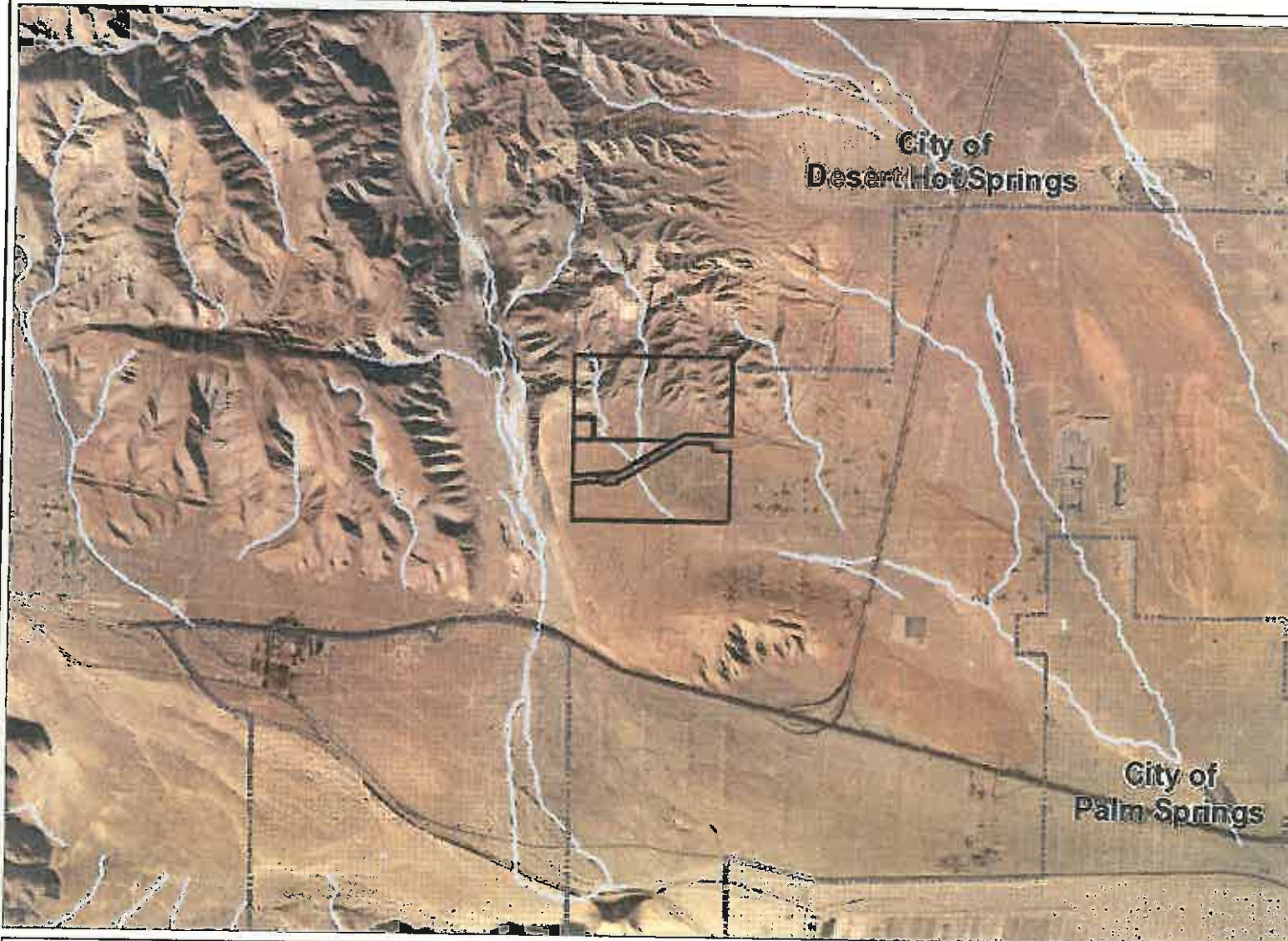


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Notes

Map My County Map



- Legend**
- Runways
 - Airports
 - Airport Influence Areas
 - Airport Compatibility Zones
 - ▨ OTHER COMPATIBILITY ZONE
 - A
 - A-EXC1
 - B1
 - B1-APZ I
 - B1-APZ I-EXC1
 - B1-APZ II
 - B1-APZ II-EXC1
 - B1-EXC1
 - B2
 - B2-EXC1
 - C
 - C1
 - C1-EXC1
 - C1-EXC3
 - C1-EXC4
 - C1-HIGHT
 - C2
 - C2-EXC1
 - C2-EXC2
 - C2-EXC3
 - C2-EXC5
 - C2-EXC8



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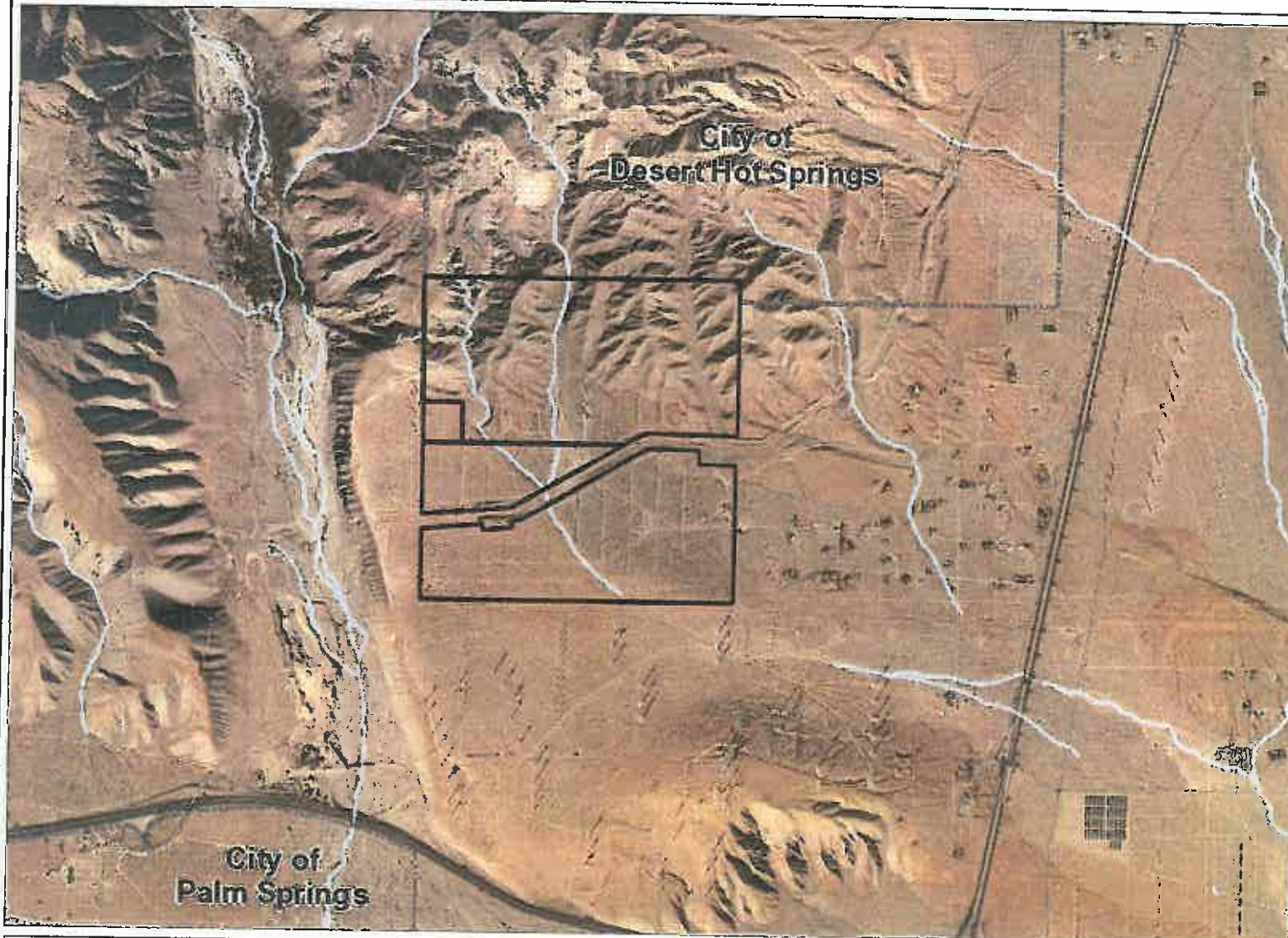


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Notes

Map My County Map



- Legend**
- Blueline Streams
 - City Areas
 - World Street Map



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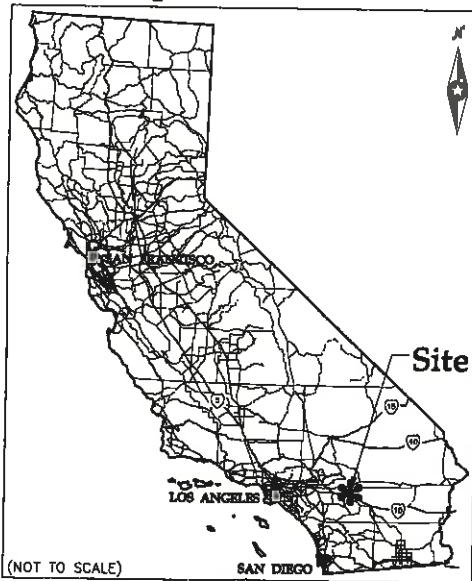
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Notes

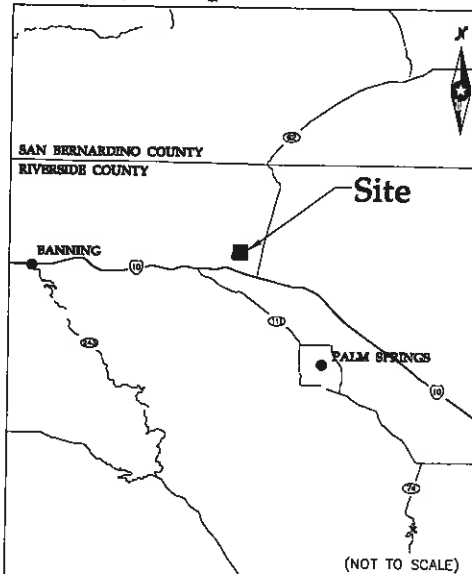
Exhibit A Site Plan Issued for Permit

for
**Painted Hills Wind Energy
Repowering Project**
Riverside County, California

State Map



Vicinity Map



PROPERTY OWNER: KATHLEEN ANN GUZINSKI
316 CORREAS ST.
HALF MOON BAY, CA 94018

JAMES D. ETDHASON
P.O. BOX 2869
PALM SPRINGS, CA 92263

JASON KEITH ETDHASON
10 W. GLENHAVEN DR.
PHOENIX, AZ 85045

SEAN D. ETDHASON
1100 ARTISTIC HEIGHTS CT.
LAS VEGAS, NV 89143

APPLICANT: PAINTED HILLS WIND, LLC
11455 EL CAMINO REAL
SUITE 100
SAN DIEGO, CA 92130

REPRESENTATIVE: WESTWOOD PROFESSIONAL SERVICES
12701 WHITEWATER DR
SUITE 200
MINNETONKA, MN 55343

PROJECT DESCRIPTION: REFER TO WECS APPLICATION

PROPERTY DATA: EXISTING ZONING: W-E

GROSS AND NET ACREAGE: 600

ASSESSOR'S PARCEL NUMBERS: 516030014, 516030008,
516030004, 516030015

UTILITIES: ELECTRIC: SOUTHERN CALIFORNIA EDISON

SITE CUT/FILL: CUT=813,500 CU. YDS.
FILL=329,820 CU. YDS.

NET=483,680 CU. YDS. CUT

EXCESS MATERIAL TO BE DISPOSED OF ON-SITE

THE PROJECT SITE FALLS WITHIN FEMA FLOOD ZONE X,
WHICH IS OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN.

THE PROJECT IS NOT LOCATED WITHIN A COMMUNITY
FACILITIES DISTRICT OR COUNTY SERVICES AREA.

LEGAL DESCRIPTION: THE LAND REFERRED TO HEREIN IS SITUATED IN THE STATE OF CALIFORNIA, COUNTY OF RIVERSIDE AND DESCRIBED AS FOLLOWS:

PARCEL A:
THE SOUTH HALF OF FRACTIONAL SECTION 1, TOWNSHIP 3 SOUTH, RANGE 3 EAST, SAN BERNARDINO BASE AND MERIDIAN, IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, ACCORDING TO THE OFFICIAL PLAT THEREOF; EXCEPT THAT PORTION OF SAID LAND DESCRIBED IN DEED IN FAVOR OF THE METROPOLITAN WATER DISTRICT OF SOUTHERN CALIFORNIA, RECORDED NOVEMBER 28, 1933 IN BOOK 148, PAGE(S) 80 OF OFFICIAL RECORDS; ALSO EXCEPT THAT PORTION OF THE EAST HALF OF THE NORTH HALF OF THE NORTH-EAST QUARTER OF THE SOUTH-EAST QUARTER OF SAID SECTION, LYING SOUTHERLY OF THE SOUTHERLY LINE OF THAT PORTION OF SAID LAND DESCRIBED IN THE DEED ABOVE MENTIONED;

PARCEL B:
THE SOUTHWEST QUARTER OF THE SOUTHWEST QUARTER OF THE NORTHWEST QUARTER OF FRACTIONAL SECTION 1, TOWNSHIP 3 SOUTH, RANGE 3 EAST, SAN BERNARDINO BASE AND MERIDIAN, IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, ACCORDING TO THE OFFICIAL PLAT THEREOF; EXCEPTING THEREFROM AN UNDIVIDED 2/3 INTEREST OF ALL MINERALS IN OR UNDER SAID LAND AS RESERVED IN DEED FROM IRA L. MOORE AND CELIA K. MOORE, RECORDED OCTOBER 28, 1960 AS INSTRUMENT NO. 93099, IN BOOK 2791, PAGE(S) 14 OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA. APN: 516-030-014 AND 516-030-008

PARCEL C:
GOVERNMENT LOTS 1, 2, 3 AND 4 AND THE SOUTH HALF OF THE NORTH HALF OF FRACTIONAL SECTION 1, TOWNSHIP 3 SOUTH, RANGE 3 EAST, SAN BERNARDINO BASE AND MERIDIAN, IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, ACCORDING TO THE OFFICIAL PLAT THEREOF; EXCEPTING THEREFROM THAT PORTION THEREOF CONVEYED TO THE METROPOLITAN WATER DISTRICT OF SOUTHERN CALIFORNIA, BY DEED RECORDED NOVEMBER 28, 1933 IN BOOK 148, PAGE(S) 80 OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA; ALSO EXCEPTING THEREFROM THE SOUTHWEST QUARTER OF THE SOUTHWEST QUARTER OF THE NORTHWEST QUARTER OF SAID SECTION; ALSO EXCEPTING THEREFROM AN UNDIVIDED 2/3 INTEREST OF ALL MINERALS IN OR UNDER SAID LAND AS RESERVED IN DEED FROM IRA L. MOORE AND CELIA K. MOORE, RECORDED OCTOBER 28, 1960 AS INSTRUMENT NO. 93099, IN BOOK 2791, PAGE(S) 14 OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA. SAID LAND IS DESCRIBED PURSUANT TO A CERTIFICATE OF PARCEL NUMBER NO. 149, RECORDED MARCH 26, 1985 AS INSTRUMENT NO. 61488 OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA. APN: 516-030-015

PROJECT CONTACT INFORMATION:

TITLE	COMPANY	NAME	CONTACT NUMBER	EMAIL
APPLICANT	PAINTED HILLS WIND, LLC	ROBERT SKAGGS	656-784-3737	rskaggs@parre-pen.com
PROJECT MANAGER	WESTWOOD PROFESSIONAL SERVICES	STEVE BATTAGLIA	852-808-7405	Steve.Battaglia@westwoodps.com
ENGINEER OF RECORD	WESTWOOD PROFESSIONAL SERVICES	CHRIS CARDA	852-808-7459	Chris.Carida@westwoodps.com
CONTRACTOR	X	X	X	X

Westwood

Phone: (805) 874-9888 12261 Wilshire Blvd., Suite 2000
Pasadena, CA 91104
Fax: (805) 874-9888
www.westwoodps.com

Date:	06/13/2018
Checked:	09
Drawn:	09
As-Built Drawn:	
Revised:	
2/1/18	

Prepared for:

**Painted Hills
Wind, LLC**
11455 El Camino Real, Suite 100
San Diego, CA 92130

**Painted Hills Wind
Energy Repowering
Project**
Riverside County, California

Cover

**ISSUED FOR PERMITTING
NOT FOR CONSTRUCTION**

Date: 06/13/2018

Sheet: 1 OF 15

LEGEND:

- EX. SECTION OR QUARTER SECTION LINE
- - - - EX. EASEMENT LINE
- - - - EX. R/W LINE
- EX. OVERHEAD POWER LINE
- EX. PROJECT 12KV OVERHEAD POWER LINE
- EX. FENCE
- EX. GRAVEL ROAD
- EX. DIRT ROAD
- EX. TURBINE STRUCTURE
- EX. POWER POLE
- EX. COMMUNICATIONS STRUCTURE
- EX. EXISTING
- R/W RIGHT-OF-WAY

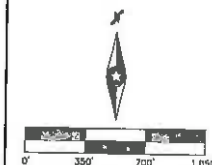
* EXISTING 12KV SCE OVERHEAD DISTRIBUTION LINE AS SHOWN MAY BE USED BY THE PROJECT AS CURRENTLY CONSTRUCTED TO INTERCONNECT SOME PROPOSED TURBINES TO THE SCE VEHWIND SUBSTATION.

Westwood

Plan: 0001814710 1271 White and Red, 04/19/2018
 Plan: 0001814710 01/19/2018, 08/28/2018
 Title: 0001814710 08/28/2018
 Westwood Professional Services, Inc.

Designed	ADP
Checked	SW
Drawn	ADP
In-Field Drawing	
Reviewed	
Approved	
Prepared for:	

**Painted Hills
Wind, LLC**
 11455 El Camino Road, Suite 160
 San Diego, CA 92120

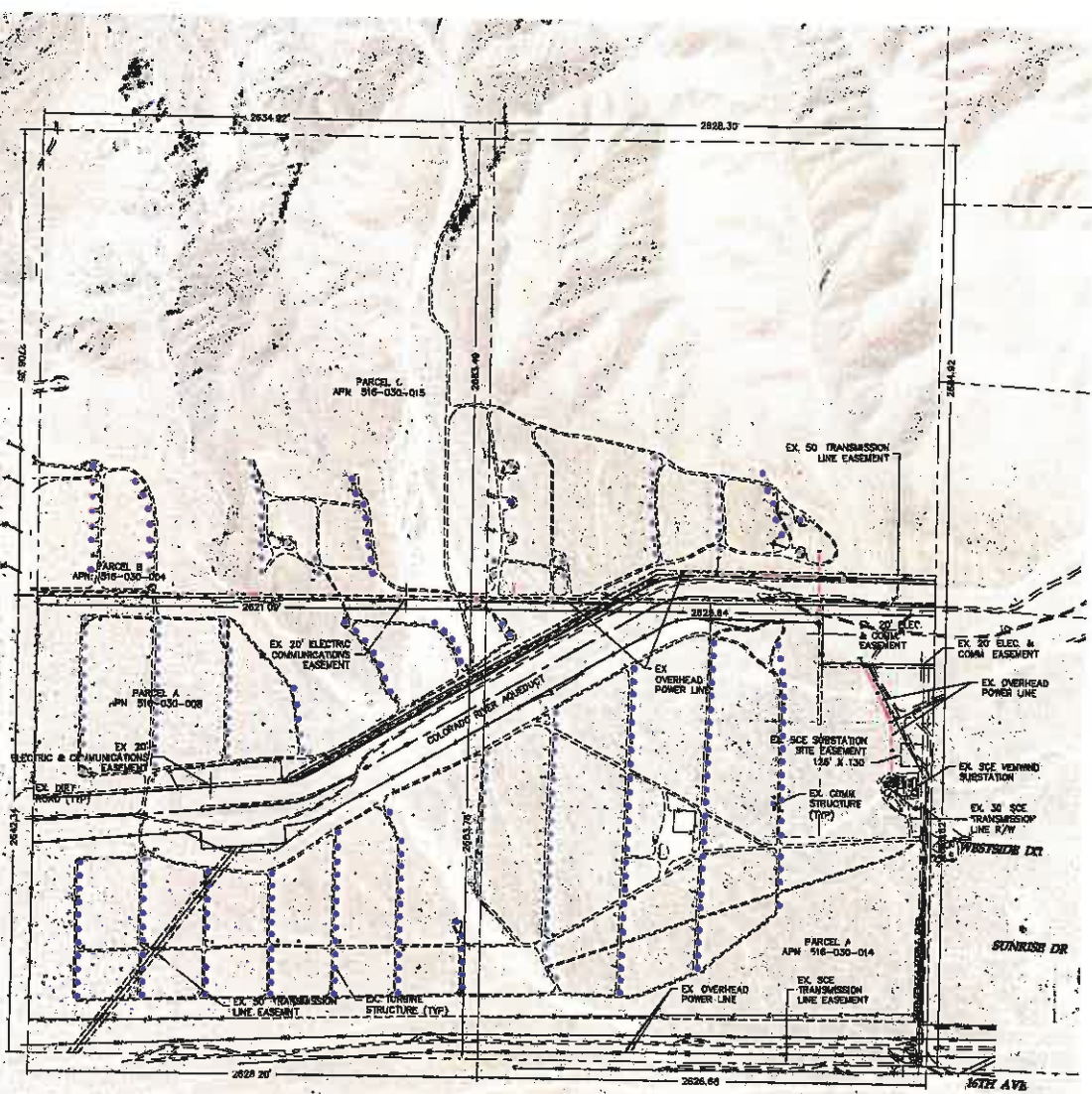


**Painted Hills Wind
Energy Repowering
Project**
 Riverside County, California

Existing Conditions

**ISSUED FOR PREMITTING
NOT FOR CONSTRUCTION**

Date: 06/13/2018
 Sheet: 2 OF 18



Attachment B: Project Description

Painted Hills Wind, LLC (Applicant) proposes to construct the Painted Hills Wind Energy Repowering Project (Project) located in western Riverside County (County), California. The Project is generally bounded by the Super Creek Mine and undeveloped foothills (i.e., Painted Hills) to the north, rural single-family residential uses and State Route (SR-) 62 to the east, existing Wind Energy Conversion System (WECS) facilities and Interstate (I-) 10 to the south, and an existing WECS facility and the unincorporated Whitewater area to the west. (see Exhibit A, Site Plan). The Project site is located within the San Geronio Wind Resource Area (SGWRA) and the County's San Geronio Pass Wind Energy Policy Area, an area that maintains winds that support economically viable wind energy projects and in which wind turbines are an established use. The Project site supports an existing wind farm and is an excellent location for generating electrical power from wind based on the strong predictable wind resource. The existing wind farm is associated with Commercial WECS Permits 25, 52, and 53.

Project Overview

The Project comprises the following components and activities:

- Decommission and remove the approximately 291 existing, antiquated turbines from the Project site.
- Install up to 14 new wind turbines and related infrastructure with a per-turbine generating capacity of between 2.0 megawatts (MW) and 4.2 MW on land within the County's Wind Energy Resource (W-E) Zone.
- Install up to 2 new permanent, lattice meteorological (met) towers to support operations of the wind development.
- Install up to 3 new temporary, guyed lattice met towers to support the power curve testing of the wind development.
- Install WECS and met tower foundations and erection of the WECS and met towers.
- Construct pad areas for individual turbines and met towers to accommodate cranes and heavy equipment needed for turbine and met tower installation.
- Construct a temporary expansion of the existing laydown yard for use during the decommissioning of existing turbines and the construction of the Project.
- Temporarily widen and improve portions of the existing internal road system.
- Construct new temporary and permanent roads outside of the existing road system footprint to accommodate cranes and heavy equipment needed for turbine and met tower installations and access to the proposed turbine and met tower foundations. Temporary new

roads and existing roads that would not be used by the Project would be restored after the construction phase and permanent new roads will be reduced to a width of 16 feet.

- Install new 12-kilovolt (kV) underground and/or overhead electrical collection lines to collect energy from the Project's new turbines. All or a portion of these lines may interconnect directly into the Southern California Edison (SCE) 115 kV Venwind substation located inside the Project boundary. Alternatively, one or more of these collection lines may tie directly into the existing, SCE-owned, 12 kV overhead collection lines inside the Project boundary that are used by the existing wind farm to interconnect into Venwind.
- Decommission and remove the new wind turbines at the end of their useful life cycle.

Project Positive Impacts

The Project would have a net positive environmental impact for the following reasons:

- The overall development intensity and visual "clutter" within the Project site would decrease as a result of the proposed repowering given that the approximately 291 existing turbines would be replaced by only 14 new turbines, which equates to a 1-to-20 (new-to-existing) replacement ratio.
- The Project would generate significantly more energy than the existing turbines operating on the Project site, which, due to their age and technology, are more inefficient and less reliable than the new turbines.
- No new buildings would be constructed, and the overall amount of impervious surfaces is not expected to increase. To the greatest extent feasible, new construction would be limited to existing disturbed and developed areas, minimizing plant and wildlife impacts.
- New, modern turbines have lower turbine rotational speeds. This, in combination with higher rotor height and greater spacing distances, would allow more visibility to avian species and increases avoidance potential.
- Fewer turbines with a wider spacing would allow for more avian passage between the turbines, eliminating the existing wind turbine wall.

Project Components

The Painted Hills Wind Energy Repowering Project ("Project") would consist of up to 14, three-bladed, upwind, horizontal-axis wind turbines ranging from 2.0 MW to 4.2 MW in nameplate capacity per turbine. Each wind turbine would be mounted on a concrete pedestal supported by a permanent concrete foundation. Each turbine would include four main physical components that would be assembled and erected during construction: the tower, the nacelle, the hub, and the three

blades. Other main turbine components include the turbine foundation, transformer, and safety features.

The turbine towers consist of three to five (depending on make, model and overall height) pre-fabricated tubular steel sections that are tapered from base to top. The Project would use turbine towers up to 94 meters (309 feet) in height. The nacelle houses equipment, including the gearbox, the electrical generator, and control equipment. It also supports the turbine blades and hub. A yaw system keeps the turbine pointed into the wind to maximize energy capture. A wind vane and anemometer are mounted at the rear of the nacelle to signal the controller with wind speed and direction information.

The three turbine blades and hub make up the rotor. The rotor is connected to the gearbox housed in the nacelle. This system powers the wind turbine. The Project would use turbines with rotor diameters of up to 130 meters (427 feet). The total height of any Project wind turbine as measured from the turbine base to the top of turbine blade in the twelve o'clock position would not exceed 152 meters (500 feet). Figure 1, Typical Wind Turbine Dimensions, provides a schematic illustration of a proposed turbine. Technical/physical specifications for the proposed turbines have been provided in the Project Description Information Sheet included in the Application for Commercial WECS.

The wind turbines would be grouped in rows and would be connected by a network of collection lines that may be partially or entirely underground. Turbines would be arranged within the row in accordance with applicable industry siting recommendations for optimum energy production and minimal land disturbance. The proposed turbine layout is shown in Exhibit A.

To promote visual continuity, all turbine structures would use uniform light-gray or off-white colors and matte finishes in conformance with FAA requirements. No reflective surfaces, logos, or markings would be used.

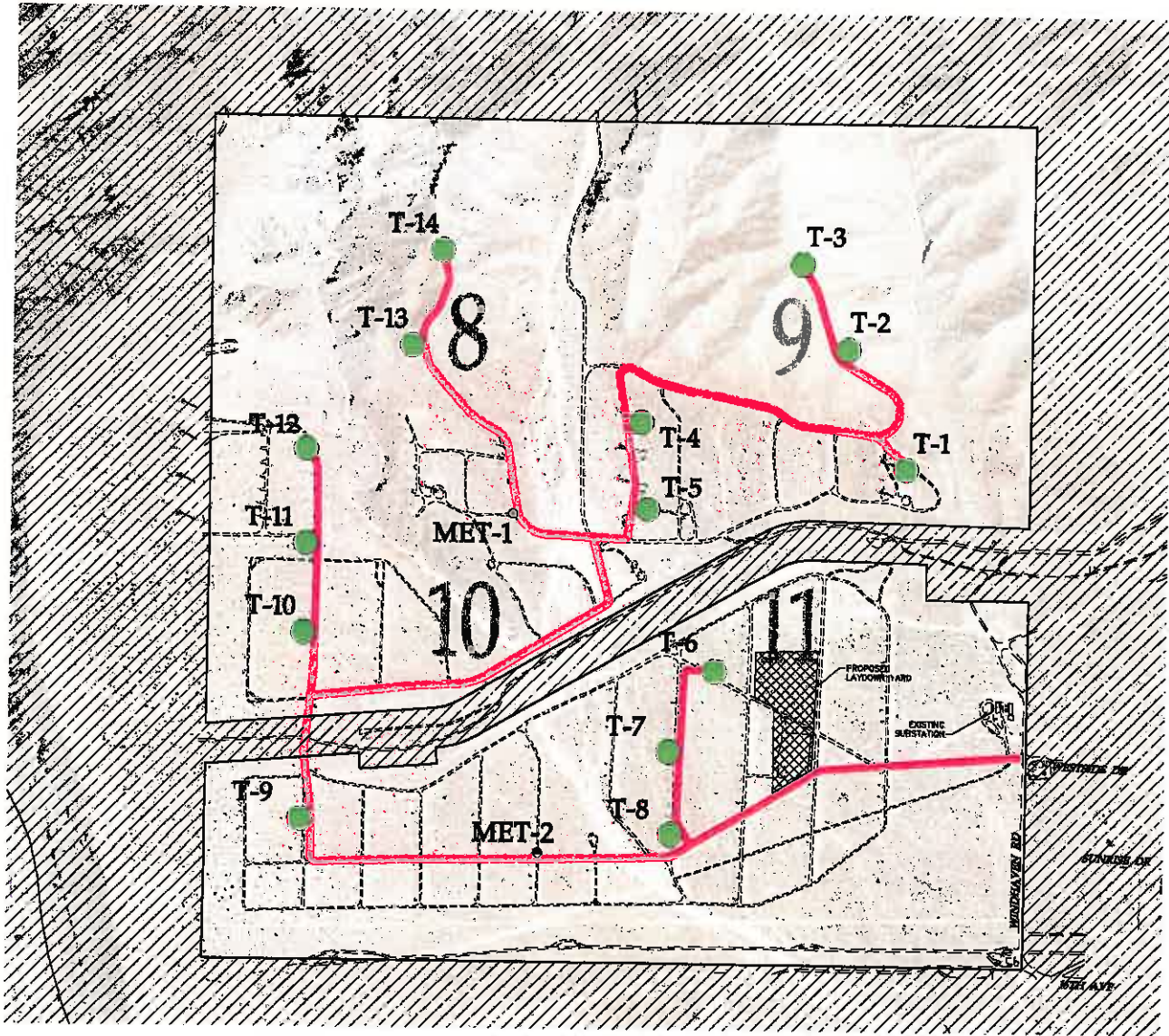
Up to two new permanent met towers would be erected within the Project site to monitor and document wind conditions. These towers would be up to 309 feet high and would be equipped with applicable FAA-compliant marking or lighting for aviation safety. Up to three new temporary met towers would also be erected within the Project site as part of the Project's wind turbine power curve testing campaign that would occur prior to commercial operations. These temporary met towers would be constructed atop targeted wind turbine locations (prior to the erection of those wind turbines) to collect turbine site specific wind data that would be used to calibrate these locations prior to performing power curve testing. These towers would also be up to 309 feet high and would be equipped with applicable FAA-compliant marking or lighting for aviation safety. The permanent met towers would be free-standing lattice towers constructed atop a concrete foundation. The temporary met towers would be guyed-lattice towers constructed atop a relatively smaller, temporary concrete foundation.

Project Operations and Maintenance

Operations and maintenance (O&M) activities for the Project would remain similar to the O&M activities conducted for the existing facility. Regularly scheduled maintenance of the Project would generally include lubrication of mechanical parts, cleaning of blades, and changing of fluids, performed in conformity with the manufacturer's guidelines. Occasionally, major overhauls or component replacements would be required, necessitating use of cranes or other equipment similar to that used during construction. Maintenance personnel would be onsite on a regular basis to service turbines, replace parts, and perform other maintenance duties.

LEGEND:

- PROPOSED TURBINE LOCATION
- T-XX** PROPOSED TURBINE NUMBER
- PROPOSED MET TOWER
- MET-XX** PROPOSED MET TOWER
- UNPAVED INTERNAL PROJECT ACCESS ROAD
- EX. GRAVEL ROAD
- EX. DIRT ROAD
- OUT OF PROJECT BOUNDARY
- 10** DENOTES SHEET NUMBER

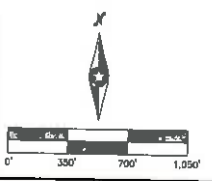


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Reviewed:	
By:	
Date:	
Prepared for:	

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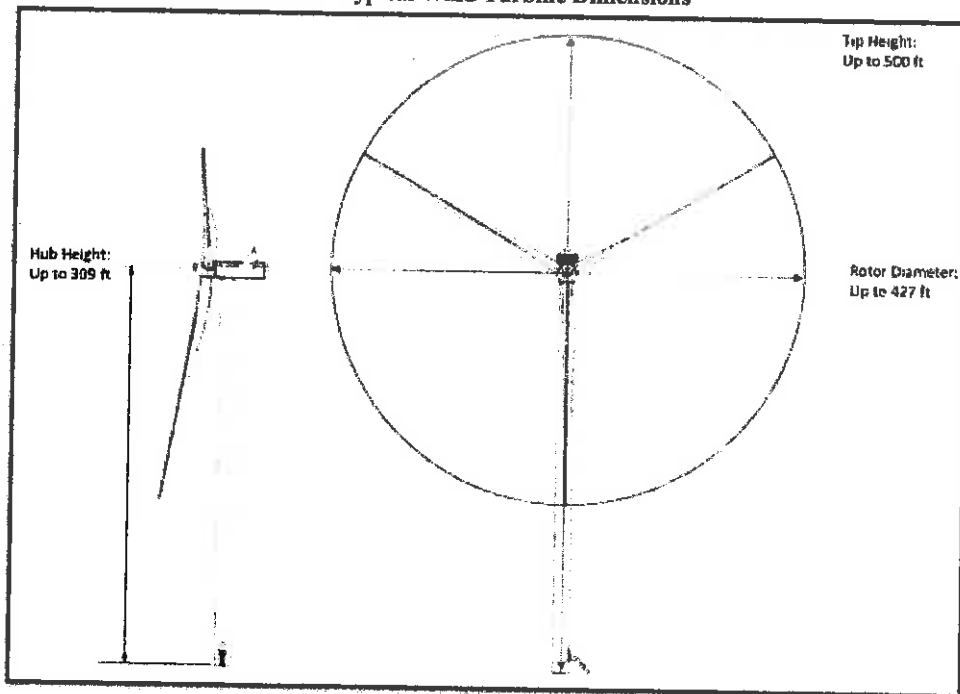
Painted Hills Wind Energy Repowering Project
 Riverside County, California

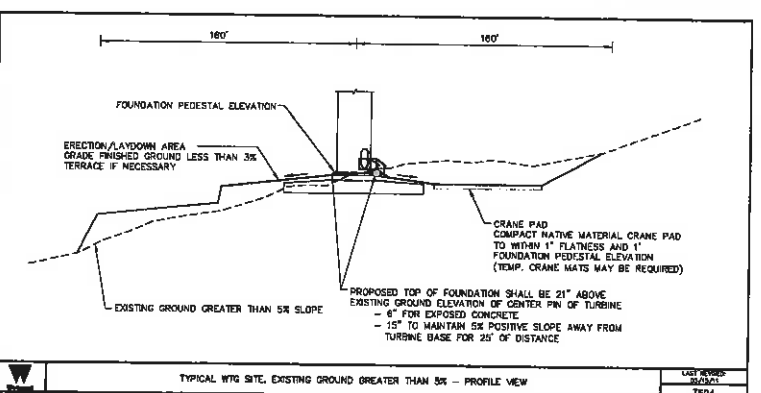
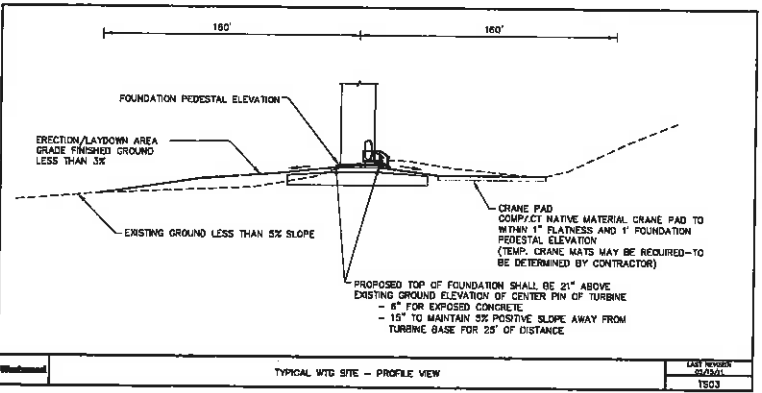
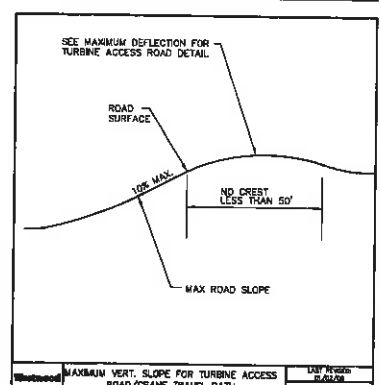
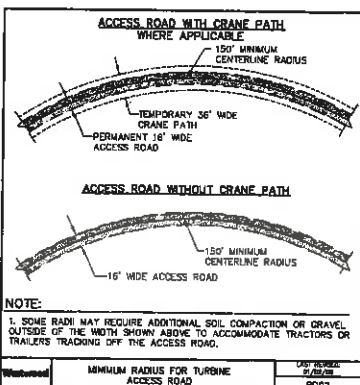
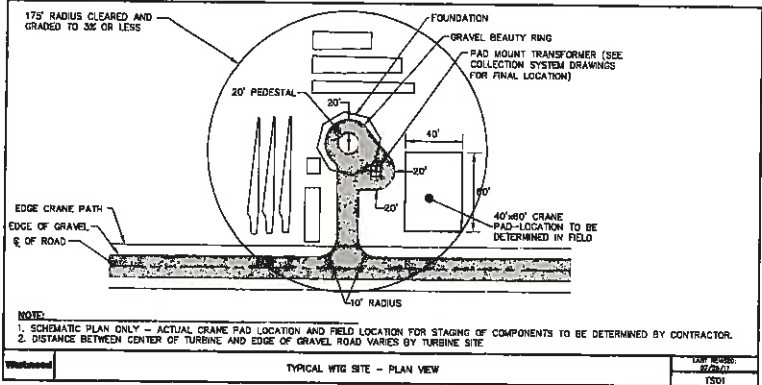
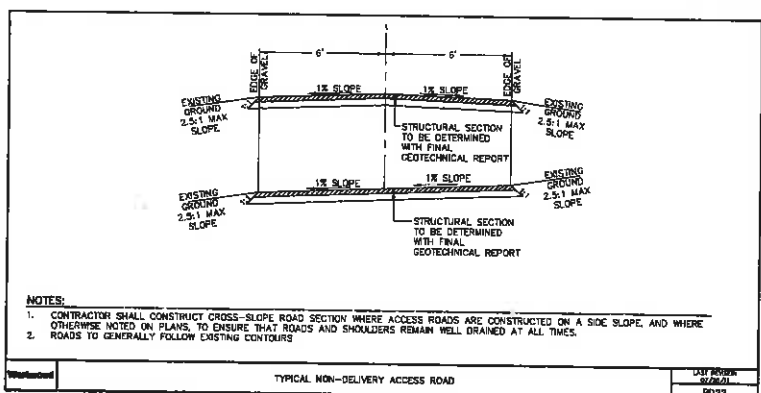
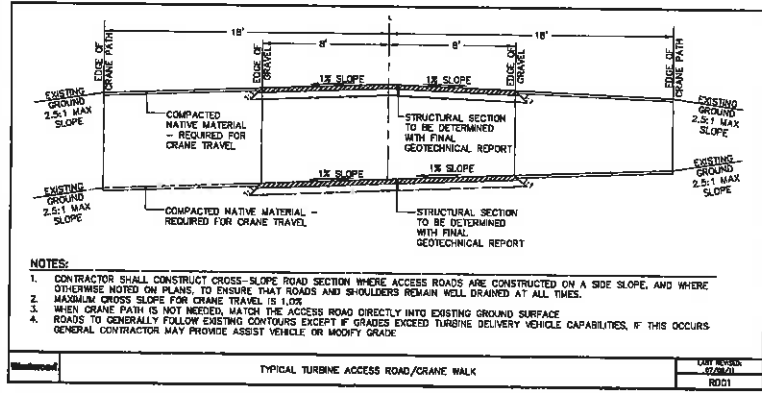
Overall Site Plan

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Date: 06/19/2015
 Sheet: 8 OF 16

Figure 1
Typical Wind Turbine Dimensions





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 Checked: RW
 Drawn: ATW
 As-built Director:
 Prepared by:
 2. TITLE DESCRIPTION

Prepared for:

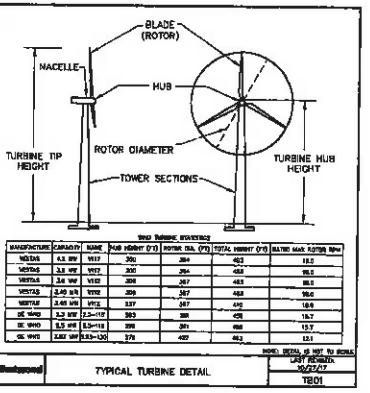
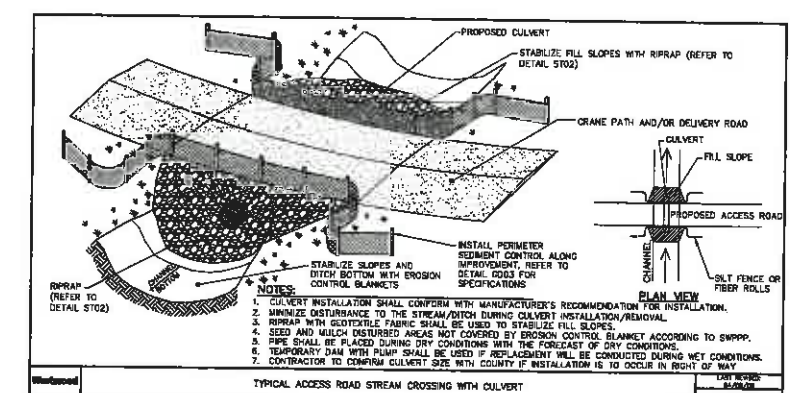
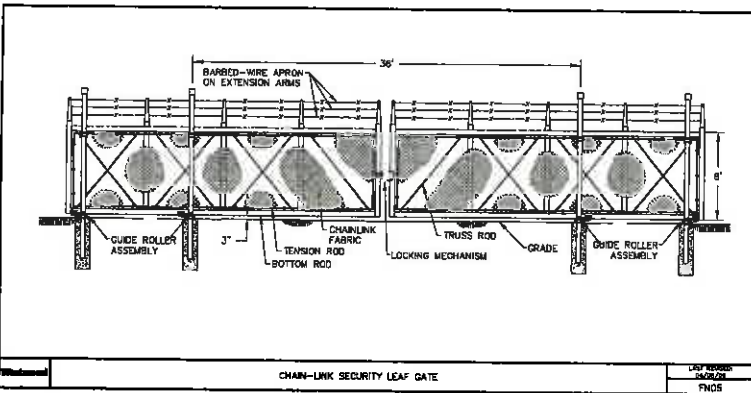
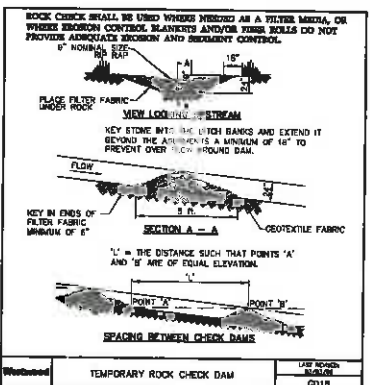
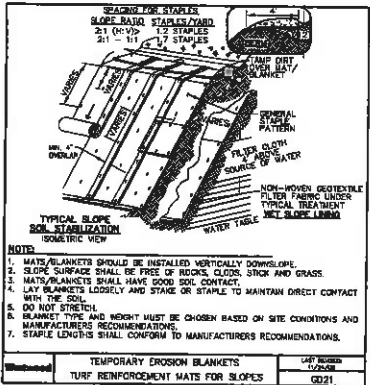
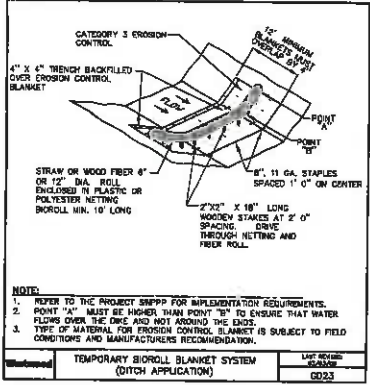
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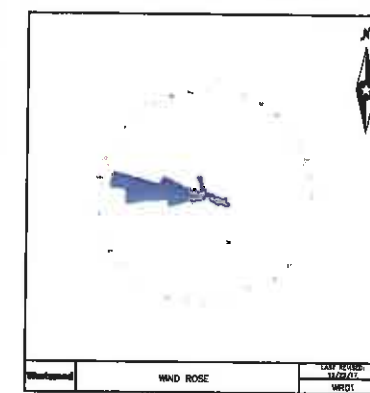
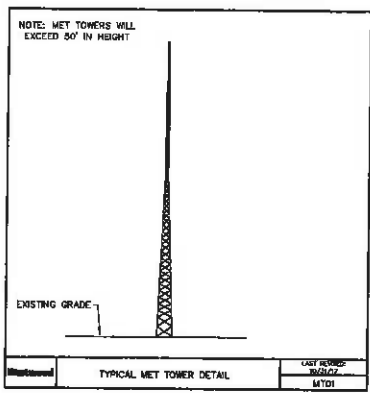
Construction Details

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Date: 06/13/2018
 Sheet: 4 OF 16



MANUFACTURER	MODEL	NAME	HUB HEIGHT (FT)	ROTOR DIA. (FT)	TURBINE TIP HEIGHT (FT)	ROTOR DIA. (FT)	TURBINE HUB HEIGHT (FT)
WINDSA	4.3 MW	W17	200	264	463	264	18.0
WINDSA	3.0 MW	W17	200	264	463	264	18.0
WINDSA	2.5 MW	W17	200	264	463	264	18.0
WINDSA	2.0 MW	W17	200	264	463	264	18.0
WINDSA	1.5 MW	W17	200	264	463	264	18.0
WINDSA	1.0 MW	W17	200	264	463	264	18.0
WINDSA	0.5 MW	W17	200	264	463	264	18.0
WINDSA	0.25 MW	W17	200	264	463	264	18.0
WINDSA	0.125 MW	W17	200	264	463	264	18.0
WINDSA	0.0625 MW	W17	200	264	463	264	18.0



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 Drawn: ATW
 As-Built Designer:
 Prepared for:

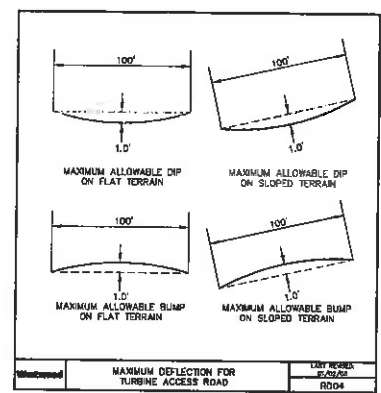
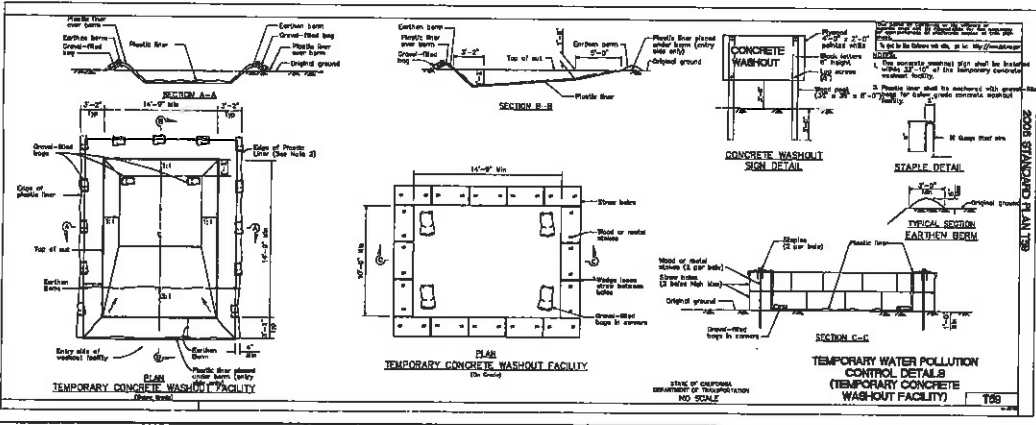
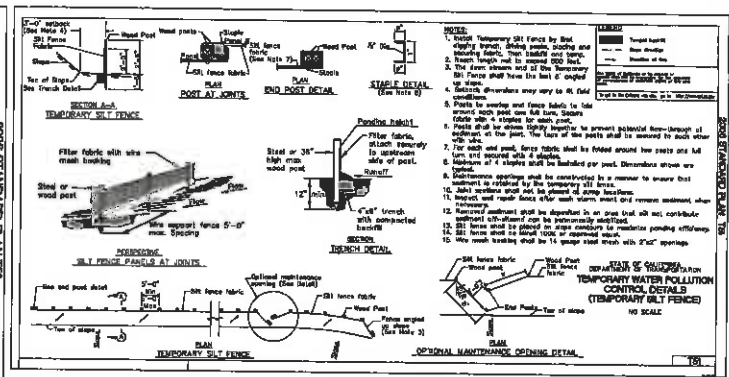
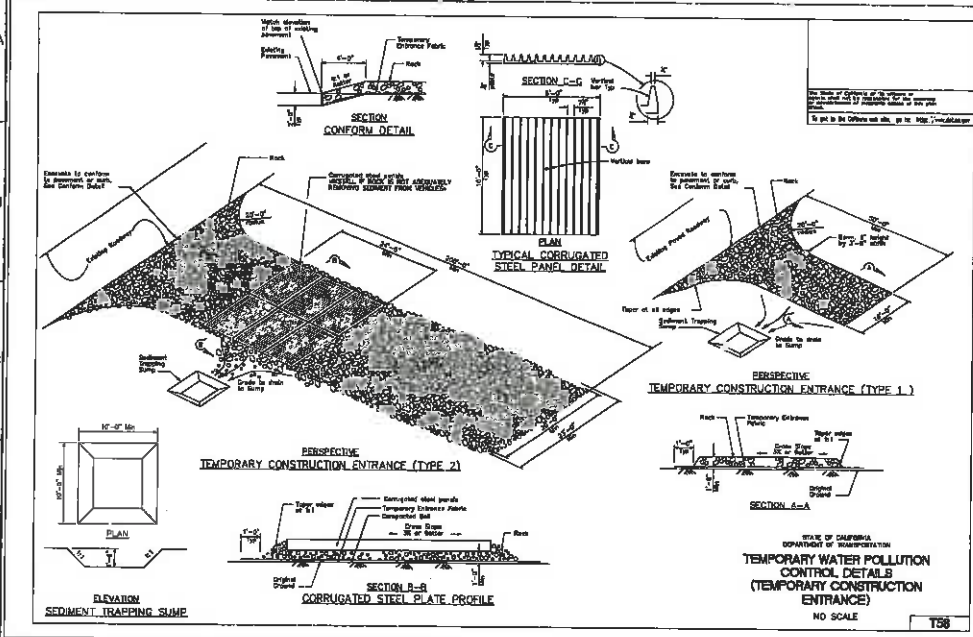
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Painted Hills Wind Energy Repowering Project
 Riverside County, California

Construction Details

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Date: 06/13/2018
 Sheet: 5 OF 18



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Painted Hills Wind Energy Repowering Project

Riverside County, California

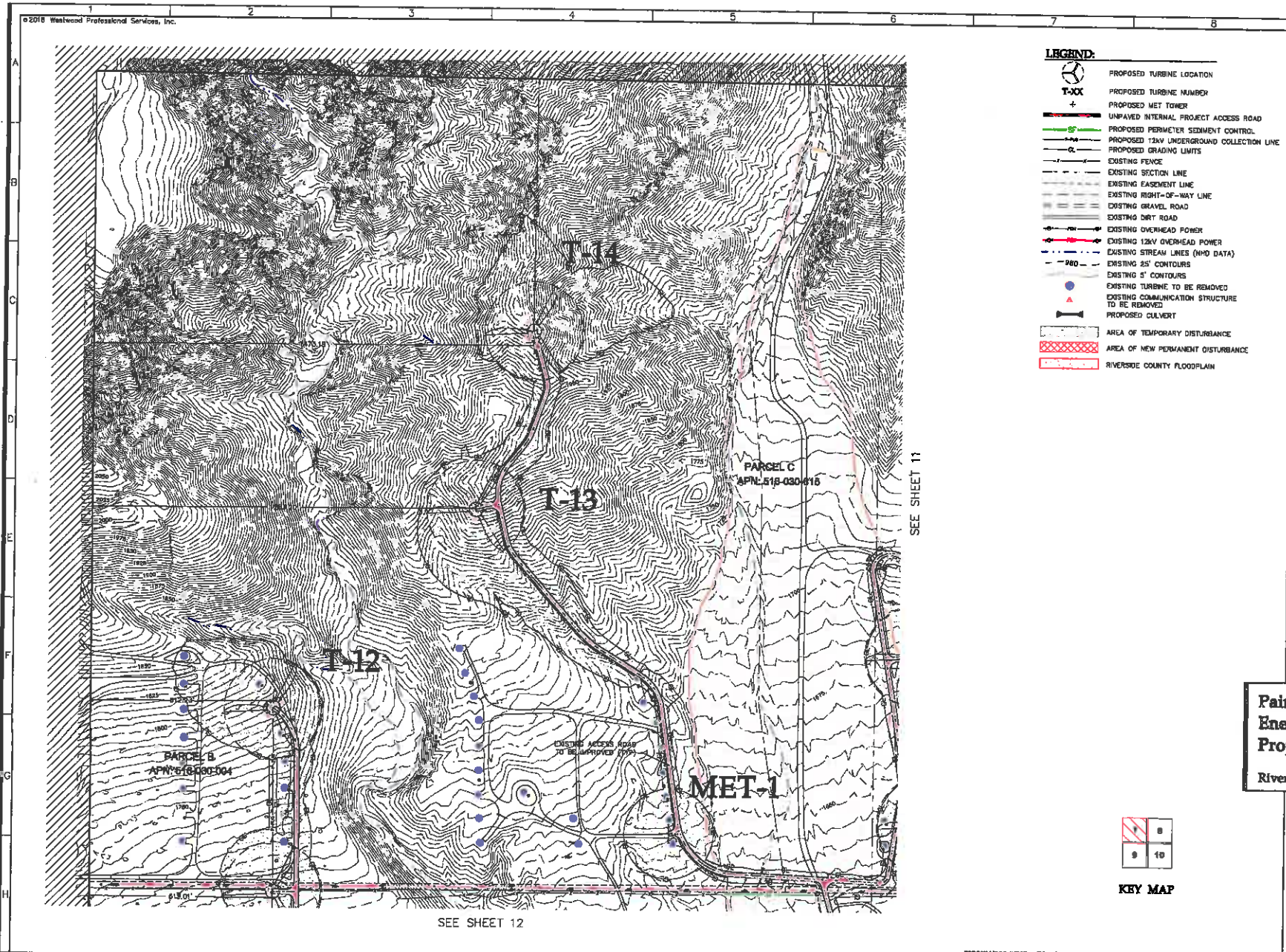
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Date: 06/13/2018

Sheet: 6 OF 18

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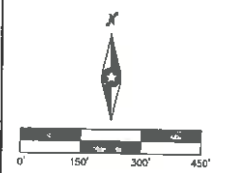
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- PROPOSED TURBINE LOCATION
 - PROPOSED TURBINE NUMBER
 - PROPOSED MET TOWER
 - UNPAVED INTERNAL PROJECT ACCESS ROAD
 - PROPOSED PERIMETER SEDIMENT CONTROL
 - PROPOSED 12kV UNDERGROUND COLLECTION LINE
 - PROPOSED GRADING LIMITS
 - EXISTING FENCE
 - EXISTING SECTION LINE
 - EXISTING EASEMENT LINE
 - EXISTING RIGHT-OF-WAY LINE
 - EXISTING GRAVEL ROAD
 - EXISTING DIRT ROAD
 - EXISTING OVERHEAD POWER
 - EXISTING 12kV OVERHEAD POWER
 - EXISTING STREAM LINES (NHD DATA)
 - EXISTING 25' CONTOURS
 - EXISTING 5' CONTOURS
 - EXISTING TURBINE TO BE REMOVED
 - EXISTING COMMUNICATION STRUCTURE TO BE REMOVED
 - PROPOSED CULVERT
 - AREA OF TEMPORARY DISTURBANCE
 - AREA OF NEW PERMANENT DISTURBANCE
 - RIVERSIDE COUNTY FLOODPLAIN

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 In Charge:

Prepared for:

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**Painted Hills Wind
 Energy Repowering
 Project**
 Riverside County, California

Civil Site Plan T-12,
 T-13, T-14, MET-1

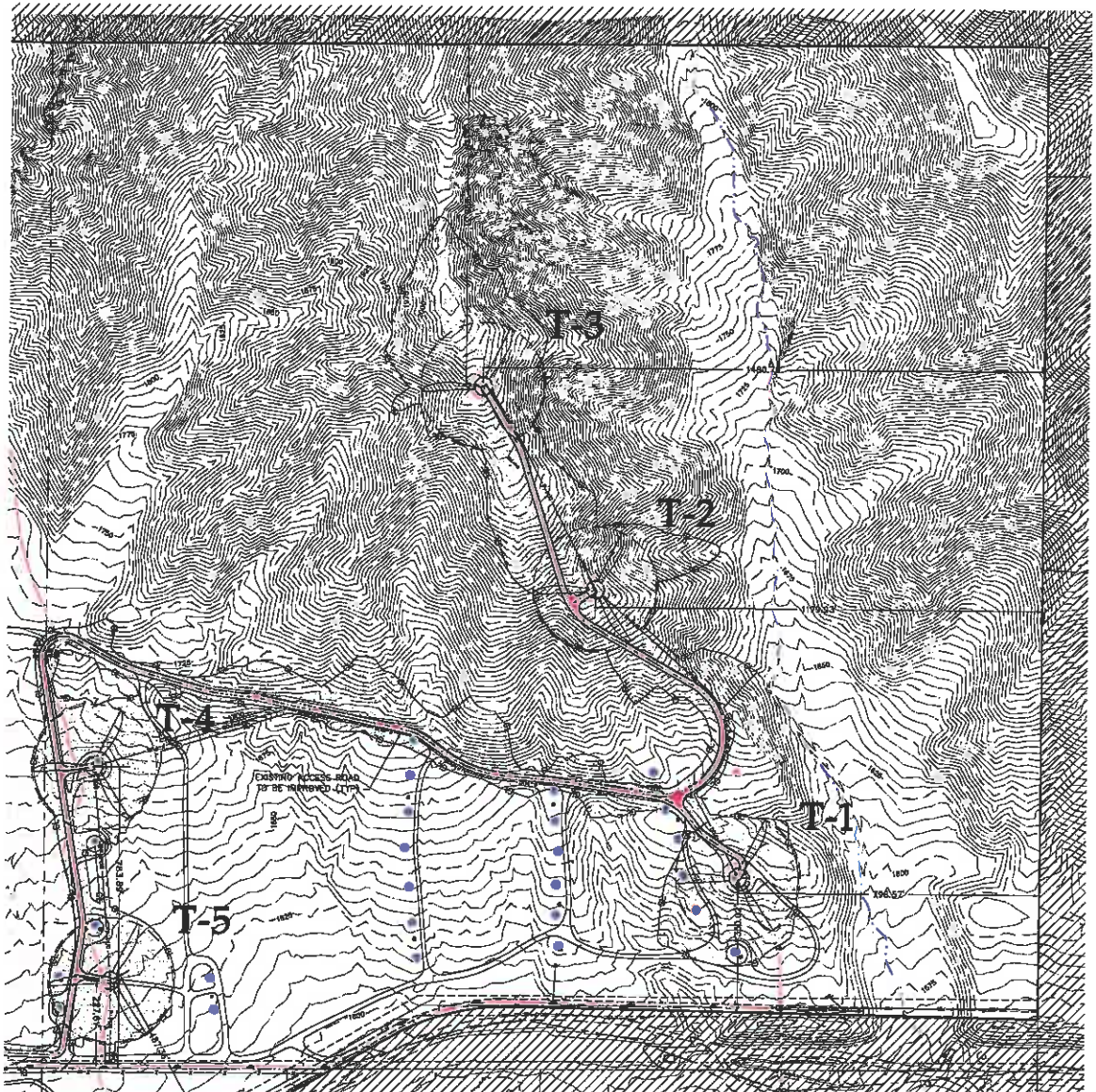
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Date: 06/13/2018
 Sheet: 7 OF 16



SEE SHEET 12

SEE SHEET 11



SEE SHEET 10

SEE SHEET 13

- LEGEND:**
- PROPOSED TURBINE LOCATION
 - PROPOSED TURBINE NUMBER
 - PROPOSED MET TOWER
 - UNPAVED INTERNAL PROJECT ACCESS ROAD
 - PROPOSED PERIMETER SEDIMENT CONTROL
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 - AREA OF TEMPORARY DISTURBANCE
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 - RIVERSIDE COUNTY FLOODPLAIN



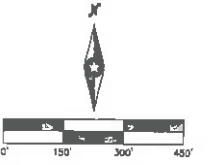
KEY MAP

Westwood
 Plans 942 357-0438 12791 Westwood Drive, Suite 1200
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Drawn:	APW
As-Built Drawing:	
Revised:	
Scale:	AS SHOWN

Prepared for:

Painted Hills Wind, LLC
 1465 W Camino Real, Suite 160
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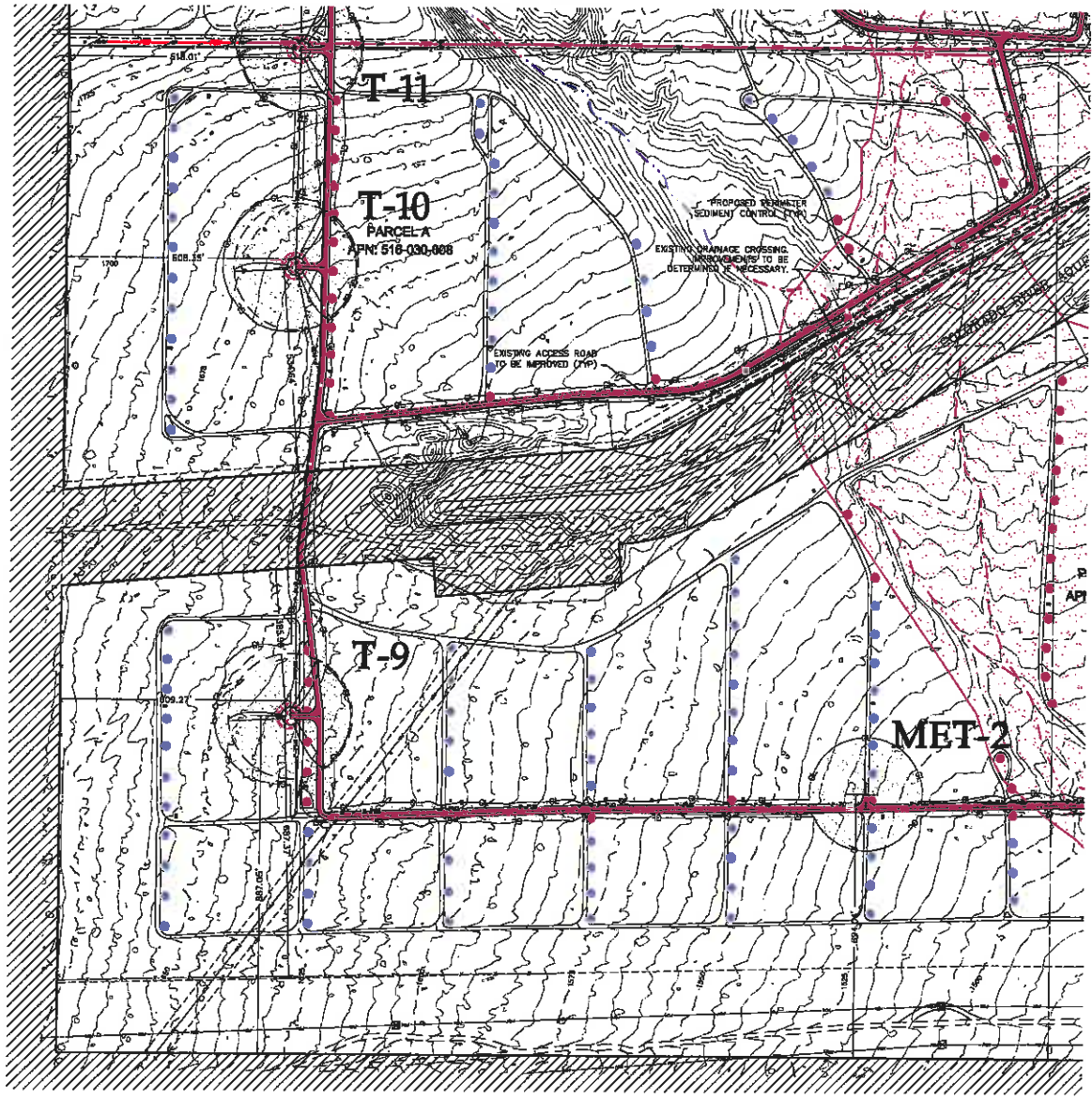
Painted Hills Wind Energy Repowering Project
 Riverside County, California

Civil Site Plan T-1, T-2, T-3, T-4, T-5

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Date: 06/13/2018
 Sheet: 8 OF 16

SEE SHEET 10



SEE SHEET 13

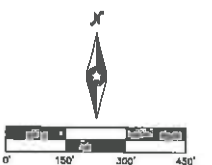
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 - AREA OF TEMPORARY DISTURBANCE
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Scale:	AS SHOWN
Date:	1/23/2018

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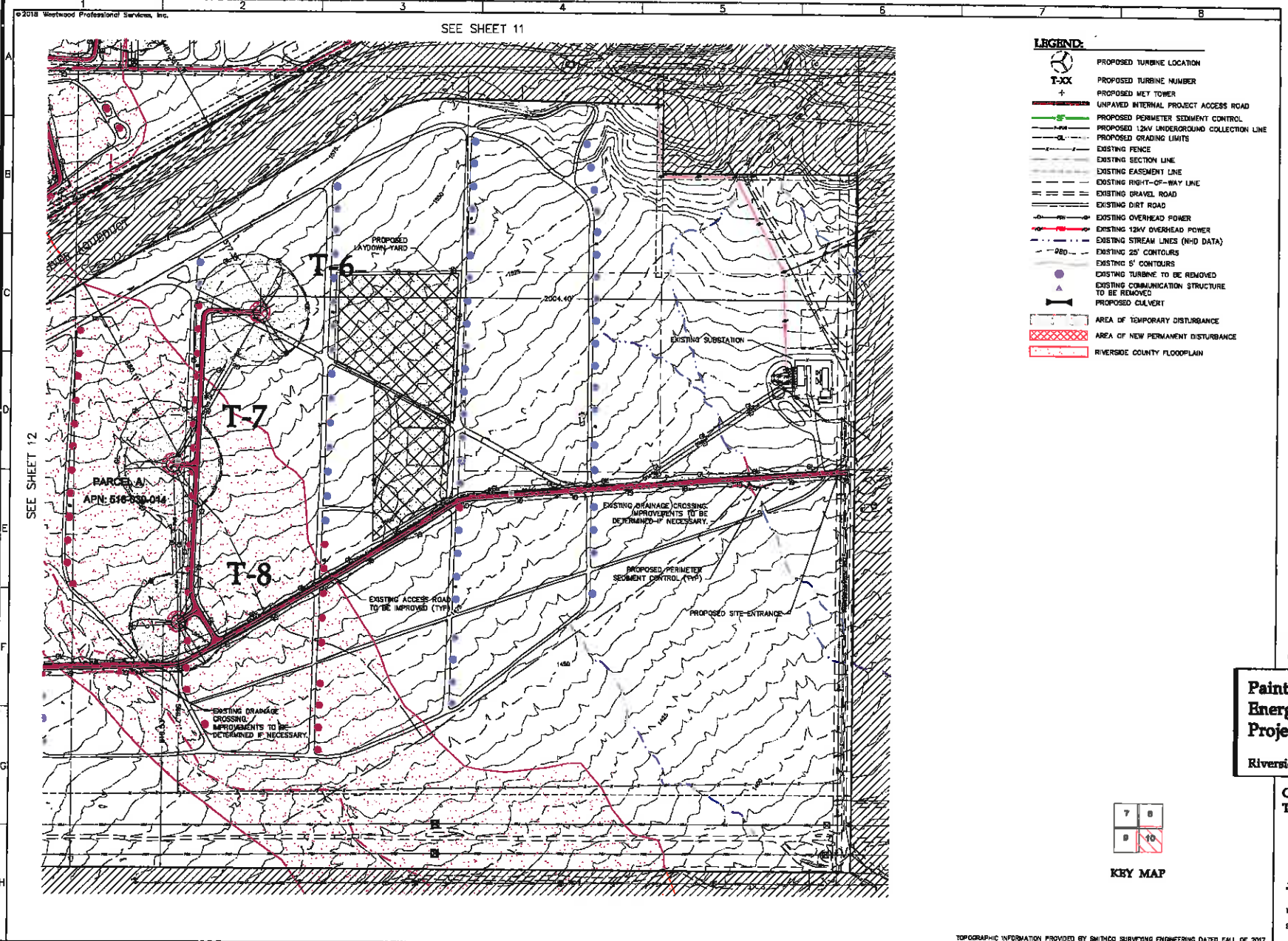
Painted Hills Wind Energy Repowering Project
 Riverside County, California



Civil Site Plan T-9, T-10, T-11, MET-2

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 Sheet: 9 OF 18



SEE SHEET 11

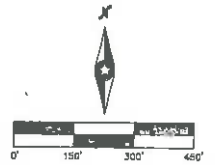
SEE SHEET 12

- LEGEND:**
- PROPOSED TURBINE LOCATION
 - PROPOSED TURBINE NUMBER
 - PROPOSED MET TOWER
 - UNPAVED INTERNAL PROJECT ACCESS ROAD
 - PROPOSED PERIMETER SEDIMENT CONTROL
 - PROPOSED 12KV UNDERGROUND COLLECTION LINE
 - PROPOSED GRADING LIMITS
 - EXISTING FENCE
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 - EXISTING GRAVEL ROAD
 - EXISTING DIRT ROAD
 - EXISTING OVERHEAD POWER
 - EXISTING 12KV OVERHEAD POWER
 - EXISTING STREAM LINES (M-D DATA)
 - EXISTING 20' CONTOURS
 - EXISTING 5' CONTOURS
 - EXISTING TURBINE TO BE REMOVED
 - EXISTING COMMUNICATION STRUCTURE TO BE REMOVED
 - PROPOSED CULVERT
 - AREA OF TEMPORARY DISTURBANCE
 - AREA OF NEW PERMANENT DISTURBANCE
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Scale:	AS SHOWN
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Painted Hills Wind Energy Repowering Project
 Riverside County, California

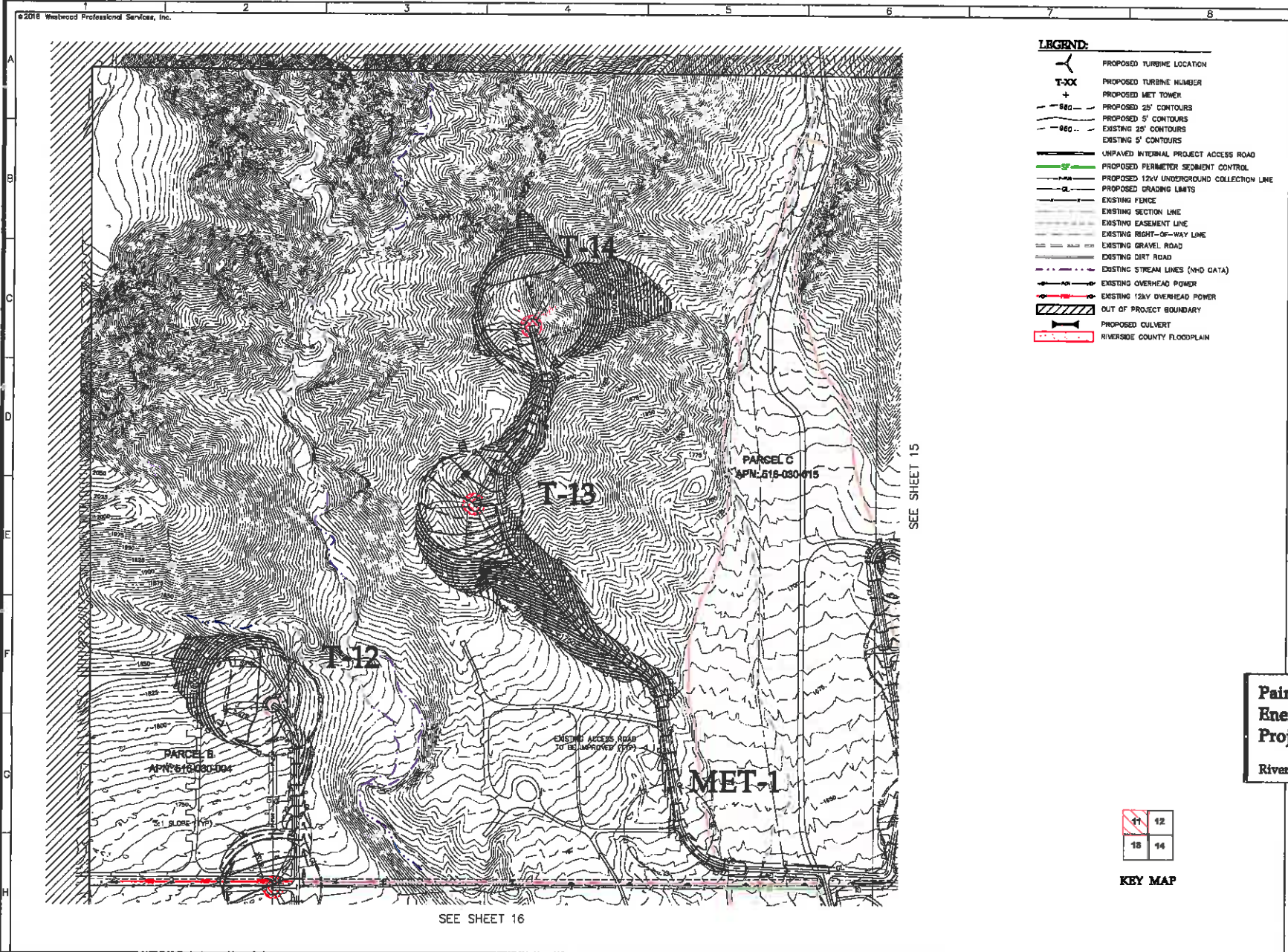


KEY MAP

Civil Site Plan T-6, T-7, T-8

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Date: 06/13/2018
 Sheet: 10 of 16



- LEGEND:**
- PROPOSED TURBINE LOCATION
 - PROPOSED TURBINE NUMBER
 - PROPOSED MET TOWER
 - PROPOSED 25' CONTOURS
 - PROPOSED 5' CONTOURS
 - EXISTING 25' CONTOURS
 - EXISTING 5' CONTOURS
 - UNPAVED INTERNAL PROJECT ACCESS ROAD
 - PROPOSED PERMETER SEDIMENT CONTROL
 - PROPOSED 12KV UNDERGROUND COLLECTION LINE
 - PROPOSED DRAINING LIMITS
 - EXISTING FENCE
 - EXISTING SECTION LINE
 - EXISTING EASEMENT LINE
 - EXISTING RIGHT-OF-WAY LINE
 - EXISTING GRAVEL ROAD
 - EXISTING DIRT ROAD
 - EXISTING STREAM LINES (MHD DATA)
 - EXISTING OVERHEAD POWER
 - EXISTING 12KV OVERHEAD POWER
 - OUT OF PROJECT BOUNDARY
 - PROPOSED CULVERT
 - RIVERSIDE COUNTY FLOODPLAIN

SEE SHEET 15

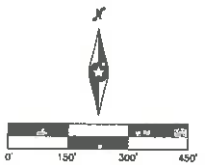
SEE SHEET 16

Westwood
 Westwood Professional Services, Inc.

Prepared for:

Designed:	AJW
Checked:	JPM
Drawn:	AJW
Auto-CAD Checked:	
Reviewed:	
Scale:	AS SHOWN

Painted Hills Wind, LLC
 11455 El Camino Real, Suite 160
 San Diego, CA 92130



Painted Hills Wind Energy Repowering Project
 Riverside County, California

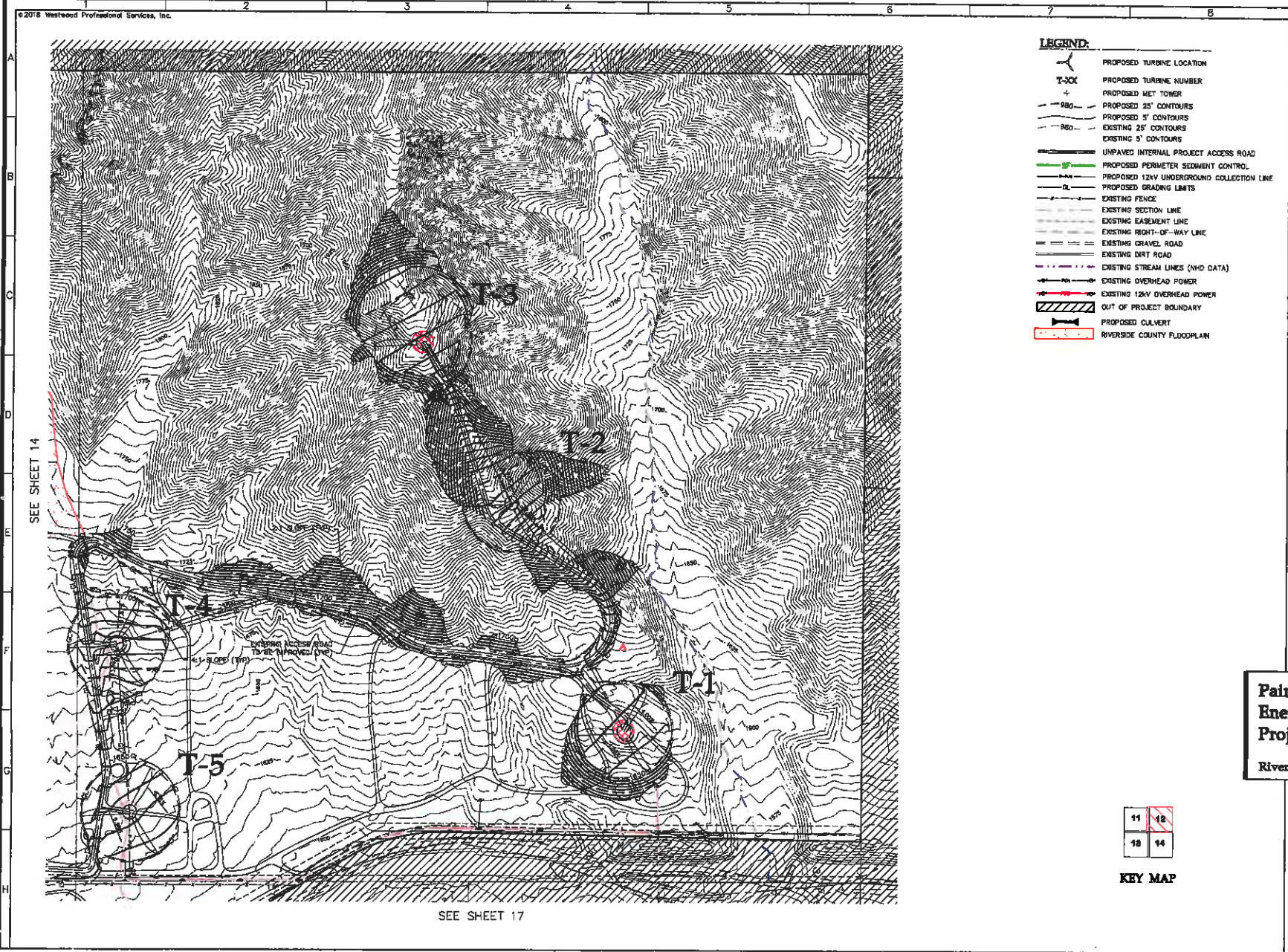
Grading Plan



KEY MAP

**ISSUED FOR PERMITTING
 NOT FOR CONSTRUCTION**

Date: 06/13/2018
 Sheet: 11 OF 15



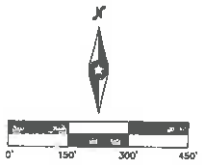
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T-XXX	PROPOSED TURBINE NUMBER
+	PROPOSED MET TOWER
- - - - -	PROPOSED 25' CONTOURS
- - - - -	PROPOSED 5' CONTOURS
- - - - -	EXISTING 25' CONTOURS
- - - - -	EXISTING 5' CONTOURS
—	UNPAVED INTERNAL PROJECT ACCESS ROAD
—	PROPOSED PERMETER SEDIMENT CONTROL
—	PROPOSED 12kV UNDERGROUND COLLECTION LINE
—	PROPOSED GRADING LIMITS
—	EXISTING FENCE
—	EXISTING SECTION LINE
—	EXISTING EASEMENT LINE
—	EXISTING RIGHT-OF-WAY LINE
—	EXISTING GRAVEL ROAD
—	EXISTING DIRT ROAD
—	EXISTING STREAM LINES (NHD DATA)
—	EXISTING OVERHEAD POWER
—	EXISTING 12kV OVERHEAD POWER
—	OUT OF PROJECT BOUNDARY
—	PROPOSED CULVERT
—	RIVERSIDE COUNTY FLOODPLAIN

Westwood
 Phone: (949) 443-9448 13751 Midway Court, Suite 200
 Fax: (949) 443-9442 Escondido, CA 92025
 Web: (949) 443-9446 www.westwoodps.com
 Westwood Professional Services, Inc.

Checked:	AJW
Checked:	MB
Drawn:	AJW
As-Built Drawn:	
Revised:	
Prepared by:	

Painted Hills Wind, LLC
 11459 El Camino Real, Suite 160
 San Diego, CA 92130



Painted Hills Wind Energy Repowering Project
 Riverside County, California



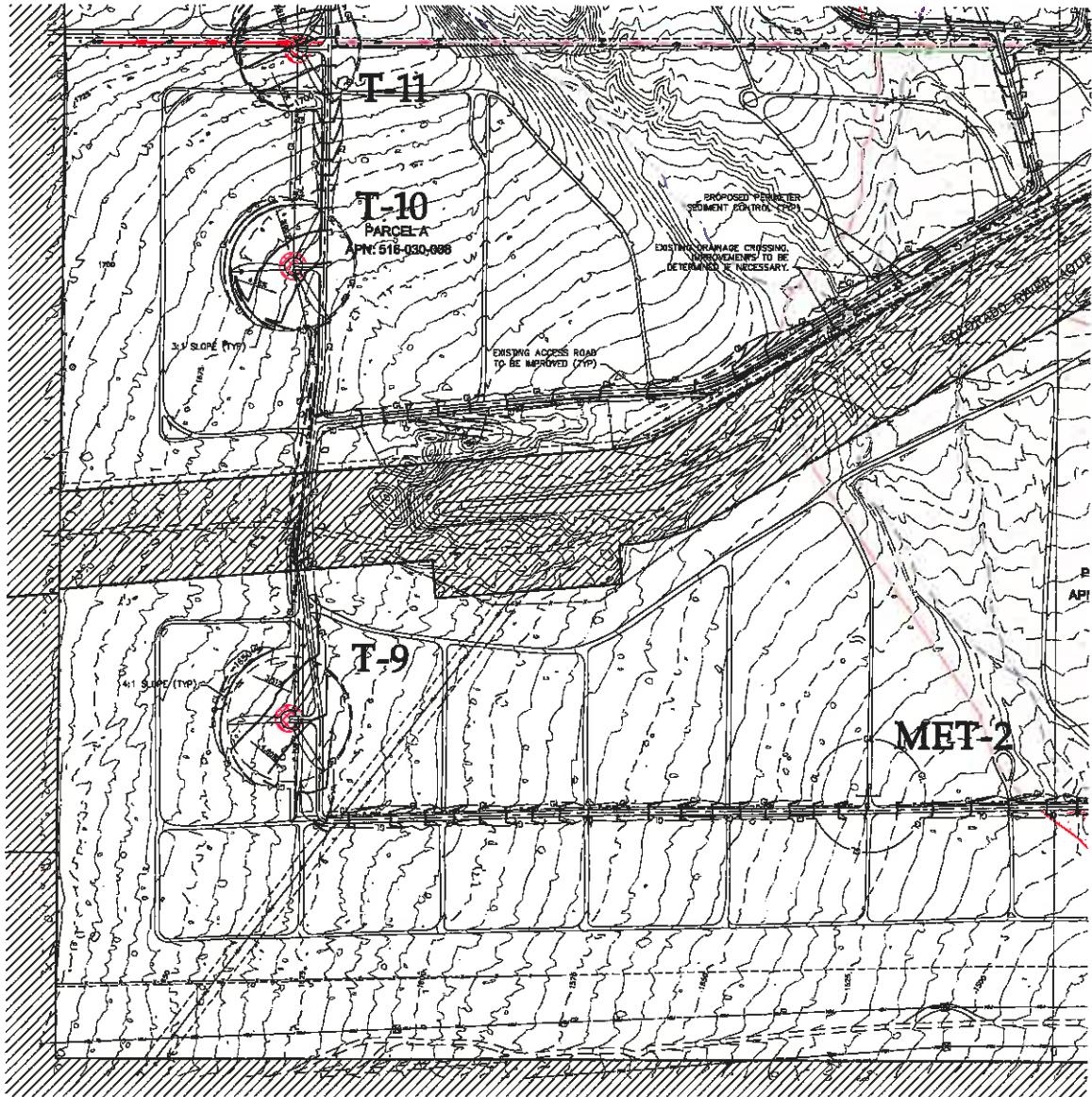
KEY MAP

Grading Plan

**ISSUED FOR PERMITTING
 NOT FOR CONSTRUCTION**

Date: 06/13/2018
 Sheet: 12 OF 16

SEE SHEET 14



SEE SHEET 17

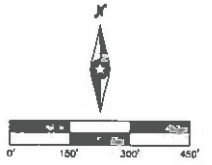
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- PROPOSED MET TOWER
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- 880- EXISTING 25' CONTOURS
- 880- EXISTING 5' CONTOURS
- UNPAVED INTERNAL PROJECT ACCESS ROAD
- PROPOSED PERIMETER SEDIMENT CONTROL
- PROPOSED 12KV UNDERGROUND COLLECTION LINE
- PROPOSED GRADING LIMITS
- EXISTING FENCE
- EXISTING SECTION LINE
- EXISTING EASEMENT LINE
- EXISTING RIGHT-OF-WAY LINE
- EXISTING GRAVEL ROAD
- EXISTING DIRT ROAD
- EXISTING STREAM LINES (MHD DATA)
- EXISTING OVERHEAD POWER
- EXISTING 12KV OVERHEAD POWER
- OUT OF PROJECT BOUNDARY
- PROPOSED CULVERT
- RIVERSIDE COUNTY FLOODPLAIN

Westwood
 Phone: (951) 837-0100 12781 Wilshire Drive, Suite 200
 Fax: (951) 837-0102 Riverside, CA 92505
 Web: (951) 837-0100 www.westwood.com
 Westwood Professional Services, Inc.

Designed:	AJM
Checked:	WP
Drawn:	AJM
As-Built Drawn:	
Revised:	
DATE:	06/13/2018
Prepared for:	

Painted Hills Wind, LLC
 11488 El Comino Blvd, Suite 360
 San Diego, CA 92130



Painted Hills Wind Energy Repowering Project
 Riverside County, California

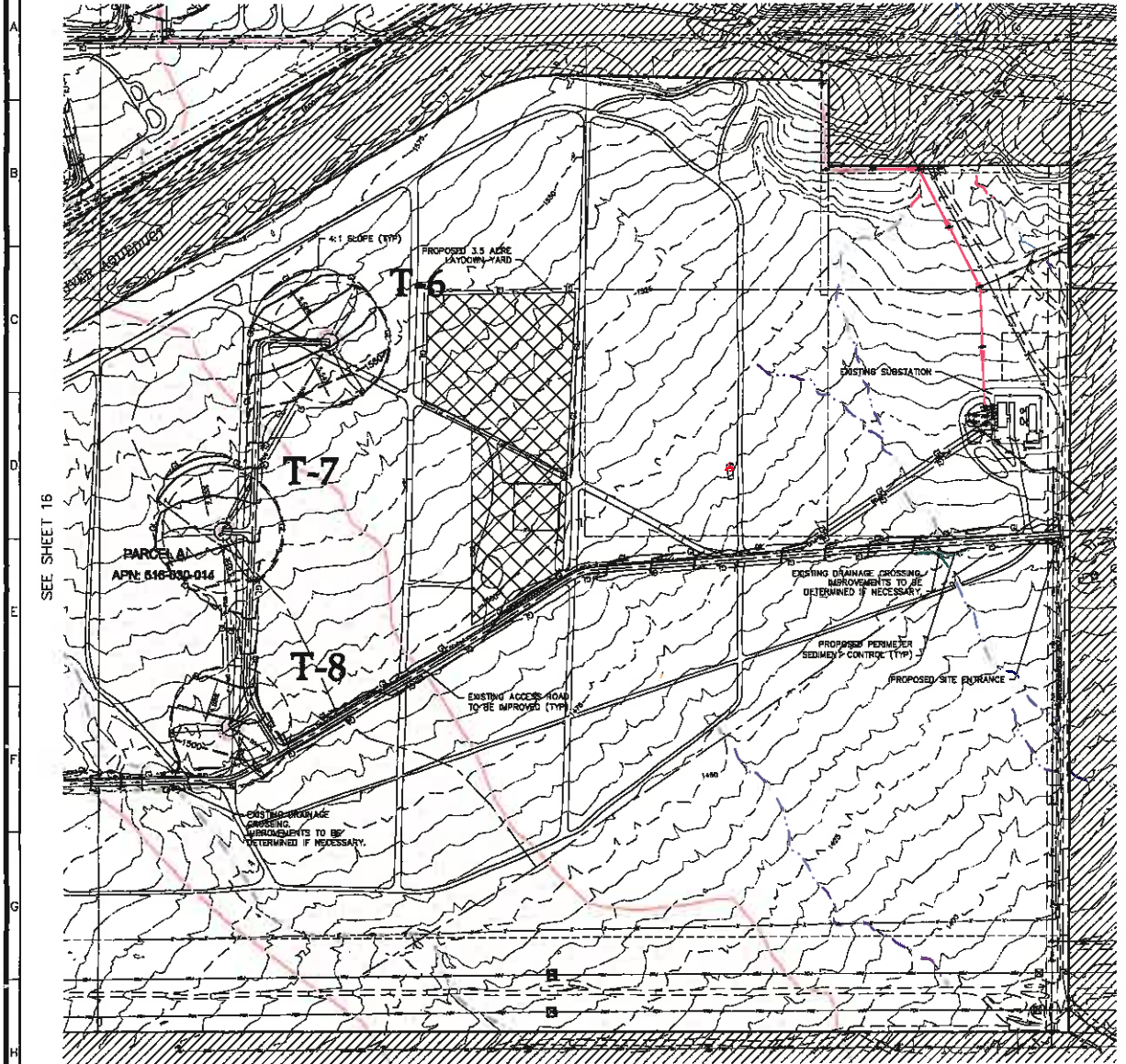
Grading Plan



KEY MAP

**ISSUED FOR PERMITTING
 NOT FOR CONSTRUCTION**

Date: 06/13/2018
 Sheet: 13 of 18



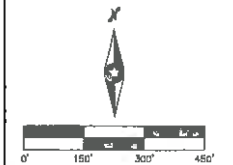
- LEGEND:**
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 - PROPOSED TURBINE NUMBER
 - PROPOSED MET TOWER
 - PROPOSED 25' CONTOURS
 - PROPOSED 5' CONTOURS
 - EXISTING 25' CONTOURS
 - EXISTING 5' CONTOURS
 - UNPAVED INTERNAL PROJECT ACCESS ROAD
 - PROPOSED PERIMETER SEDIMENT CONTROL
 - PROPOSED 12kV UNDERGROUND COLLECTION LINE
 - PROPOSED GRADING LIMITS
 - EXISTING FENCE
 - EXISTING SECTION LINE
 - EXISTING EASEMENT LINE
 - EXISTING RIGHT-OF-WAY LINE
 - EXISTING GRAVEL ROAD
 - EXISTING DIRT ROAD
 - EXISTING STREAM LINES (W/O DATA)
 - EXISTING OVERHEAD POWER
 - EXISTING 12kV OVERHEAD POWER
 - OUT OF PROJECT BOUNDARY
 - PROPOSED CULVERT
 - RIVERSIDE COUNTY FLOODPLAIN

Westwood
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Designed:	JW
Checked:	JW
Drawn:	JW
As-Built Drawings:	
Revisions:	
1	2

Prepared for:

Painted Hills Wind, LLC
 2455 El Camino Real, Suite 501
 San Diego, CA 92108



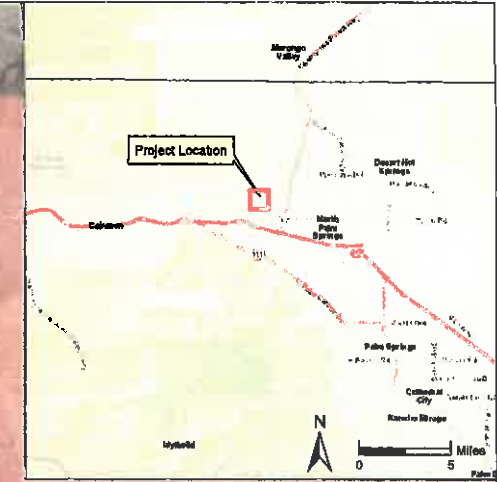
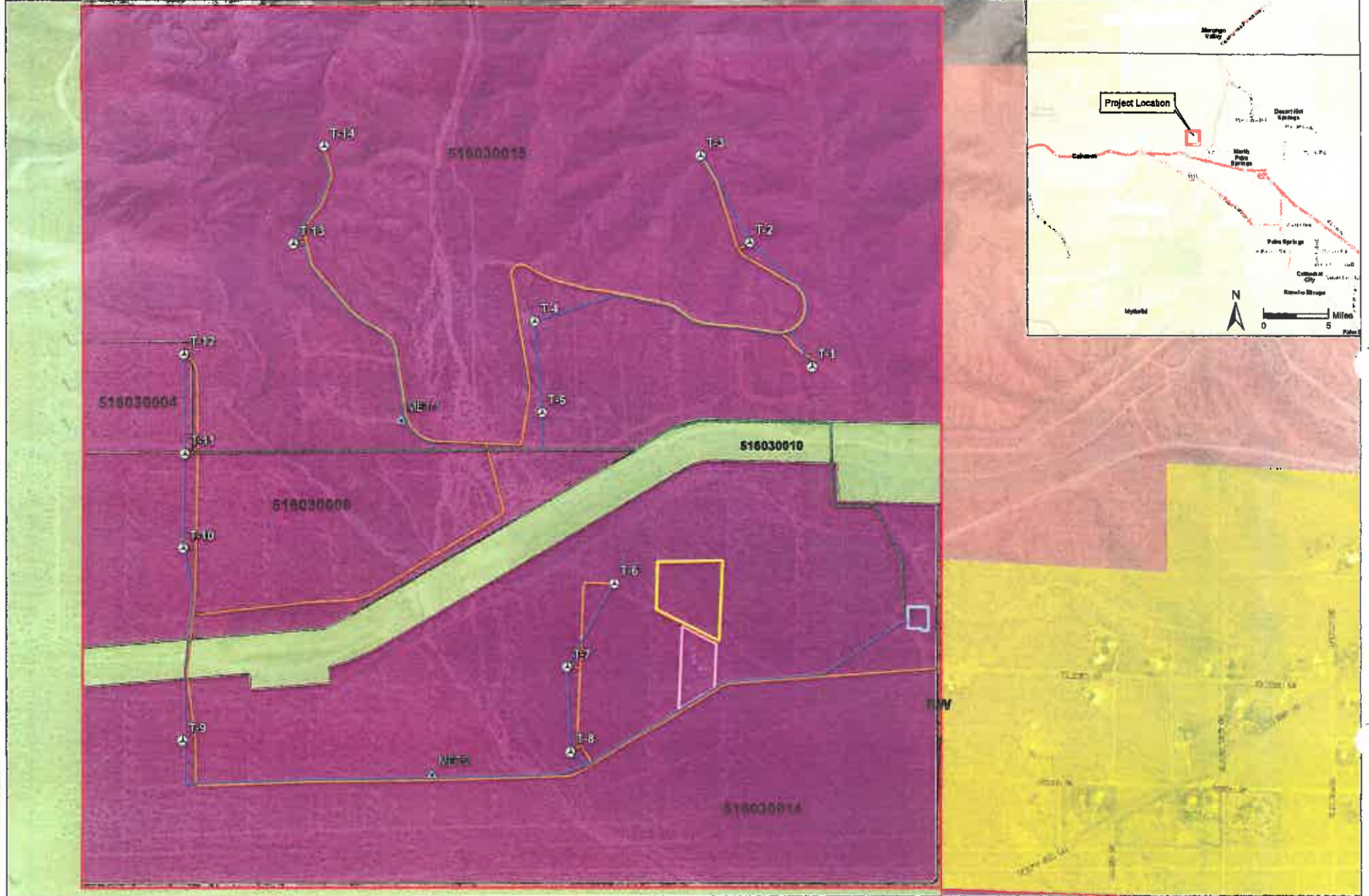
Painted Hills Wind Energy Repowering Project
 Riverside County, California

Grading Plan



ISSUED FOR PREMITTING
 NOT FOR CONSTRUCTION

Date: 06/13/2018
 Sheet: 14 OF 15
 002470-001PW22.dwg



Date Source(s): ESRI, Westwood (2018).

- Legend**
- MET Towers
 - Proposed Turbines
 - Existing Overhead Collection Line
 - UG Collection Line
 - Project Boundary
 - Project Access Roads
 - Existing Laydown Yard to Remain
 - Laydown Yard Expansion
 - Parcels
 - Existing Substation to Remain
 - R-1
 - R-R
 - W-2
 - W-E

Painted Hills Wind Energy Repowering Project

Riverside County, California

THE FEDERAL EMERGENCY MANAGEMENT AGENCY FLOOD RATE MAP FOR COMMUNITY NO. 06065C, PANEL NOS. 0670G, 0880G AND 0890G, DATED AUGUST 28, 2008, SHOWS THAT THE PROJECT SITE FALLS WITHIN ZONE X, WHICH IS OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN



Painted Hills Wind Energy Repowering Project

Painted Hills Wind, LLC

Riverside County, California

Obstruction Evaluation & Airspace Analysis

March 9, 2018



Capitol Airspace Group

capitolairspace.com

(703) 256 - 2485

Summary

Capitol Airspace conducted an obstruction evaluation and airspace analysis for the Painted Hills Wind Energy Repowering Project in Riverside County, California. The purpose for this analysis was to identify obstacle clearance surfaces established by the Federal Aviation Administration (FAA) that could limit the placement of 499 foot above ground level (AGL) wind turbines. At the time of this analysis, 14 individual wind turbine locations had been identified (black points, [Figure 1](#)). This analysis assessed height constraints overlying each location, as well as an approximately 1.3 square mile study area (red outline, [Figure 1](#)) to aid in identifying optimal wind turbine locations.

14 CFR Part 77.9 requires that all structures exceeding 200 feet AGL be submitted to the FAA so that an aeronautical study can be conducted. The FAA's objective in conducting aeronautical studies is to ensure that proposed structures do not have an effect on the safety of air navigation and the efficient utilization of navigable airspace by aircraft. The end result of an aeronautical study is the issuance of a determination of 'hazard' or 'no hazard' that can be used by the proponent to obtain necessary local construction permits. It should be noted that the FAA has no control over land use in the United States and cannot enforce the findings of its studies.

Height constraints overlying the Painted Hills Wind Energy Repowering Project range from 2,421 to 2,591 feet above mean sea level (AMSL) and are associated with instrument departure procedures. Proposed wind turbines that exceed this surface would require an increase to instrument departure procedure minimum climb gradients. If the FAA determines this impact to constitute a substantial adverse effect, it could be used as the basis for determinations of hazard. However, United States Geological States (USGS) elevation data indicates that it surface should not limit 499 foot AGL wind turbines at any of the proposed locations.

This analysis did not consider electromagnetic interference on communication, navigation, or radar surveillance systems.

Capitol Airspace applies FAA defined rules and regulations applicable to obstacle evaluation, instrument procedures assessment and visual flight rules (VFR) operations to the best of its ability and with the intent to provide the most accurate representation of limiting airspace surfaces as possible. Capitol Airspace maintains datasets obtained from the FAA which are updated on a 56 day cycle. The results of this analysis/map are based on the most recent data available as of the date of this report. Limiting airspace surfaces depicted in this report are subject to change due to FAA rule changes and regular procedure amendments. Therefore, it is of the utmost importance to obtain FAA determinations of no hazard prior to making substantial financial investments in this project.

Methodology

Capitol Airspace studied the proposed project based upon location information provided by Painted Hills Wind, LLC. Using this information, Capitol Airspace generated graphical overlays to determine proximity to airports (**Figure 1**), published instrument procedures, FAA minimum vectoring altitude and minimum instrument flight rules (IFR) altitude charts, enroute airways, and military airspace and training routes.

Capitol Airspace evaluated all 14 CFR Part 77 imaginary surfaces, published instrument approach and departure procedures, visual flight rules operations, FAA minimum vectoring altitudes, minimum IFR altitudes, and enroute operations. All formulas, headings, altitudes, bearings and coordinates used during this study were derived from the following documents and data sources:

- 14 CFR Part 77 Safe, Efficient Use, and Preservation of the Navigable Airspace
- FAA Order 7400.2L Procedures for Handling Airspace Matters
- FAA Order 8260.3D United States Standard for Terminal Instrument Procedures
- FAA Order 8260.58A United States Standard for Performance Based Navigational (PBN) Instrument Procedure Design
- United States Government Flight Information Publication, US Terminal Procedures
- National Airspace System Resource Aeronautical Data

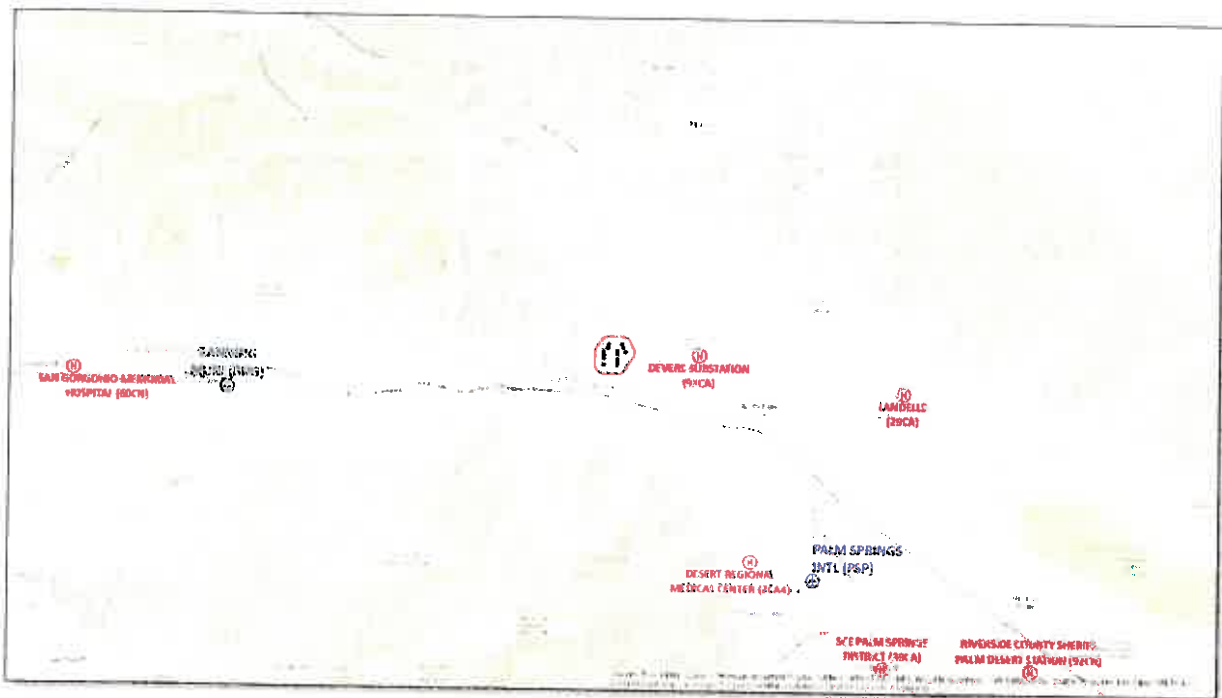


Figure 1: Public-use airports (blue) and private-use heliports (red) in proximity to the Painted Hills Wind Energy Repowering Project

Study Findings

14 CFR Part 77 Imaginary Surfaces

The FAA uses level and sloping imaginary surfaces to determine if a proposed structure is an obstruction to air navigation. Structures that are identified as obstructions are then subject to a full aeronautical study and increased scrutiny. However, exceeding a Part 77 imaginary surface does not automatically result in the issuance of a determination of hazard. Proposed structures must have airspace impacts that constitute a substantial adverse effect in order to warrant the issuance of determinations of hazard.

Public-use airport 14 CFR Part 77.17(a)(2) and 77.19/21/23 imaginary surfaces do not overlie the Painted Hills Wind Energy Repowering Project (*Figure 2*). As a result, 499 foot AGL wind turbines will not exceed these surfaces. However, if the planned wind turbine height is increased so that it exceeds 14 CFR Part 77.17(a)(1) – a height of 499 feet at the site of the object – all wind turbines will be identified to be obstructions regardless of their location.

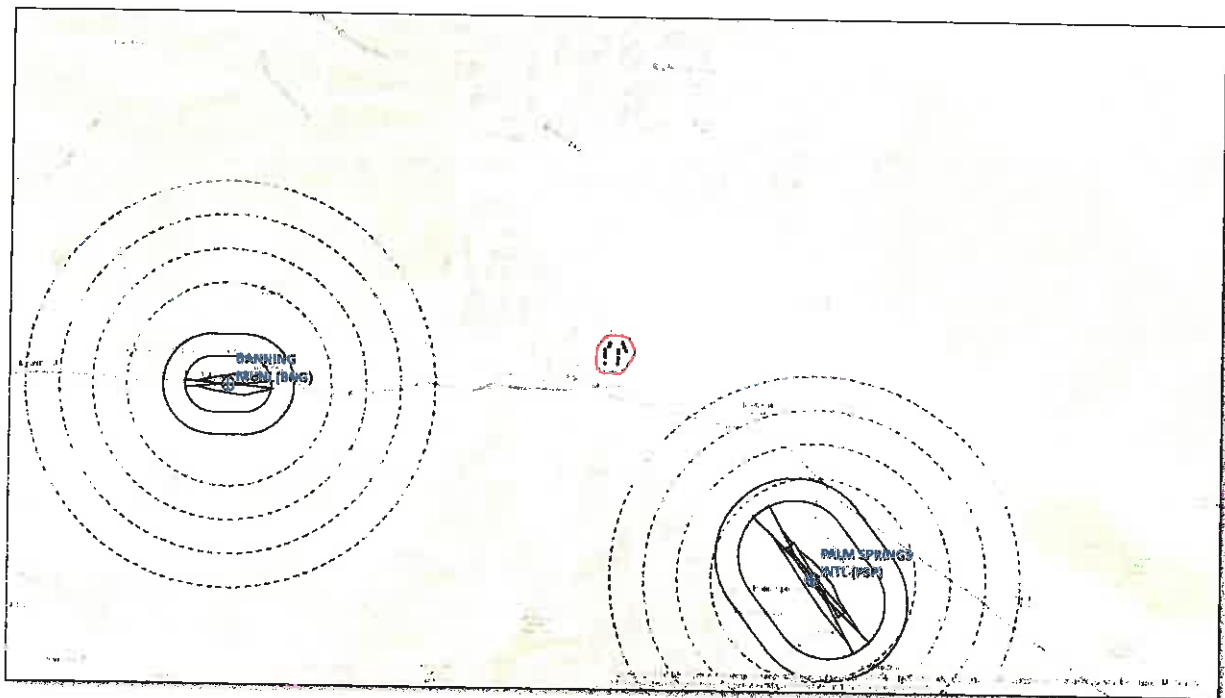


Figure 2: Public-use airport 14 CFR Part 77.17(a)(2) (dashed blue) and 77.19 (black) imaginary surfaces in proximity to the Painted Hills Wind Energy Repowering Project

Visual Flight Rules (VFR) Traffic Pattern Airspace

VFR traffic pattern airspace is used by pilots operating during visual meteorological conditions. The airspace dimensions are based upon the category of aircraft which, in turn, is based upon the approach speed of the aircraft. 14 CFR Part 77.17(a)(2) and 77.19 (as applied to a *visual* runway) imaginary surfaces establish the obstacle clearance surface heights within VFR traffic pattern airspace.

VFR traffic pattern airspace does not overlie the Painted Hills Wind Energy Repowering Project and should not limit 499 foot AGL wind turbines at any of the proposed locations (**Figure 3**).

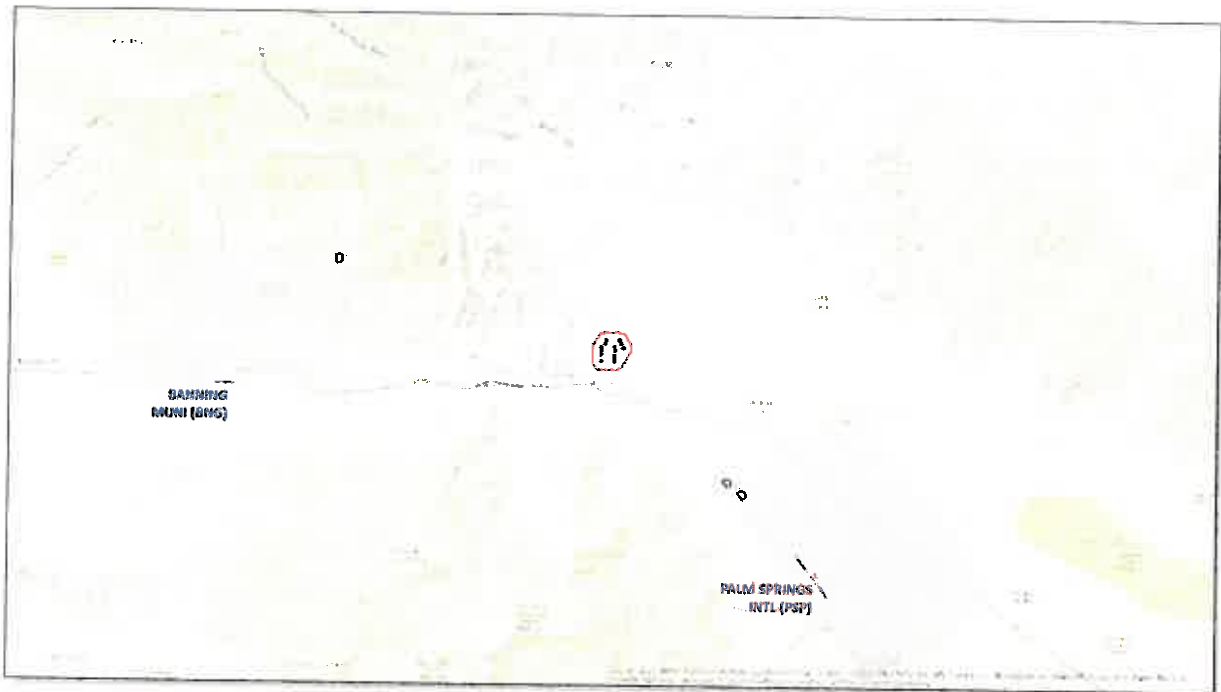


Figure 3: VFR traffic pattern airspace in proximity to the Painted Hills Wind Energy Repowering Project

Instrument Departures

In order to ensure that aircraft departing during marginal weather conditions do not fly into terrain or obstacles, the FAA publishes instrument departure procedures that provide obstacle clearance to pilots as they transition between the terminal and enroute environments. These procedures contain specific routing and minimum climb gradients to ensure clearance from terrain and obstacles.

Proposed structures that exceed instrument departure procedure obstacle clearance surfaces would require an increase to minimum climb gradients and/or change to routing. If the FAA determines that this impact would constitute a substantial adverse effect, it could be used as the basis for determinations of hazard.

Palm Springs International Airport

Runway 31L Cathedral One Standard Instrument Departure (SID) (Figure 4)

Obstacle clearance surfaces range from 2,421 to 2,591 feet AMSL where they overlie the study area and is the lowest height constraint overlying the entire study area. USGS elevation data indicates that these surfaces could limit 499 foot AGL wind turbines on higher terrain in a few small areas within the study area (red areas, [Figure 4](#)). However, none of the proposed wind turbines are located in these areas.

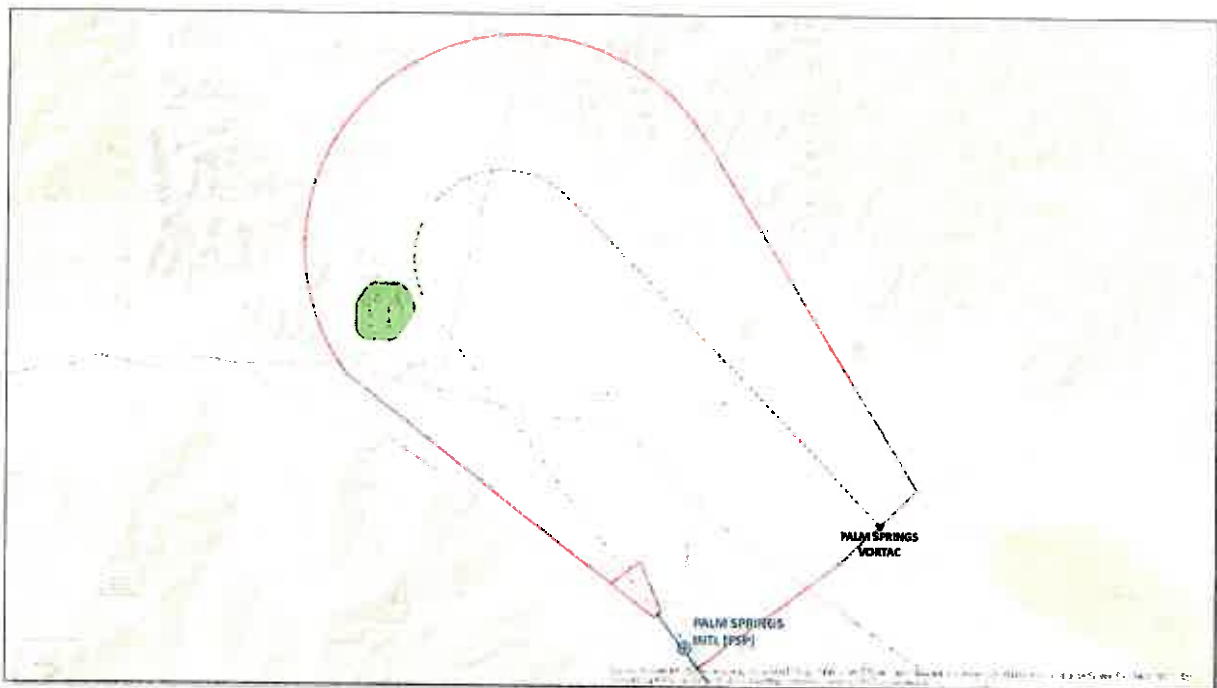


Figure 4: Palm Springs International Airport (PSP) Runway 31L Cathedral One departure

Instrument Approaches¹

Pilots operating during periods of reduced visibility and low cloud ceilings rely on terrestrial and satellite based navigational aids (NAVAIDS) in order to navigate from one point to another and to locate runways. The FAA publishes instrument approach procedures that provide course guidance to on-board avionics that aid the pilot in locating the runway. Capitol Airspace assessed a total of eight published instrument approach procedures at four public-use airports in proximity to the Painted Hills Wind Energy Repowering Project:

Bermuda Dunes (UDD)

RNAV (GPS) Approach to Runway 10
RNAV (GPS) Approach to Runway 28
VOR-C Circling Approach

Palm Springs International (PSP)

RNAV (RNP) Y Approach to Runway 31L
RNAV (RNP) Z Approach to Runway 13R
VOR or GPS-B Circling Approach

Big Bear City (L35)

RNAV (GPS) Approach to Runway 26

Hemet-Ryan (HMT)

RNAV (GPS) Approach to Runway 05

Proposed wind turbines that exceed instrument approach procedure obstacle clearance surfaces would require an increase to their minimum altitudes. Increases to these altitudes, especially *critical decision altitudes (DA)* and *minimum descent altitudes (MDA)*, can directly impact the efficiency of instrument approach procedures. If the FAA determines this impact to constitute a substantial adverse effect it could be used as the basis for determinations of hazard.

Instrument approach procedure obstacle clearance surfaces either do not overlie the study area (e.g., [Figure 5](#)) or are in excess of other lower surfaces and should not limit 499 foot AGL wind turbines at any of the proposed locations.

¹ Palm Springs International Airport has multiple special instrument approach procedures. Since special instrument approach procedure documentation is not publicly available, Capitol Airspace was unable to assess for associated height constraints. If these procedures are routed over the study area it is possible that their associated obstacle clearance surfaces could be lower than the other surfaces described in this report.

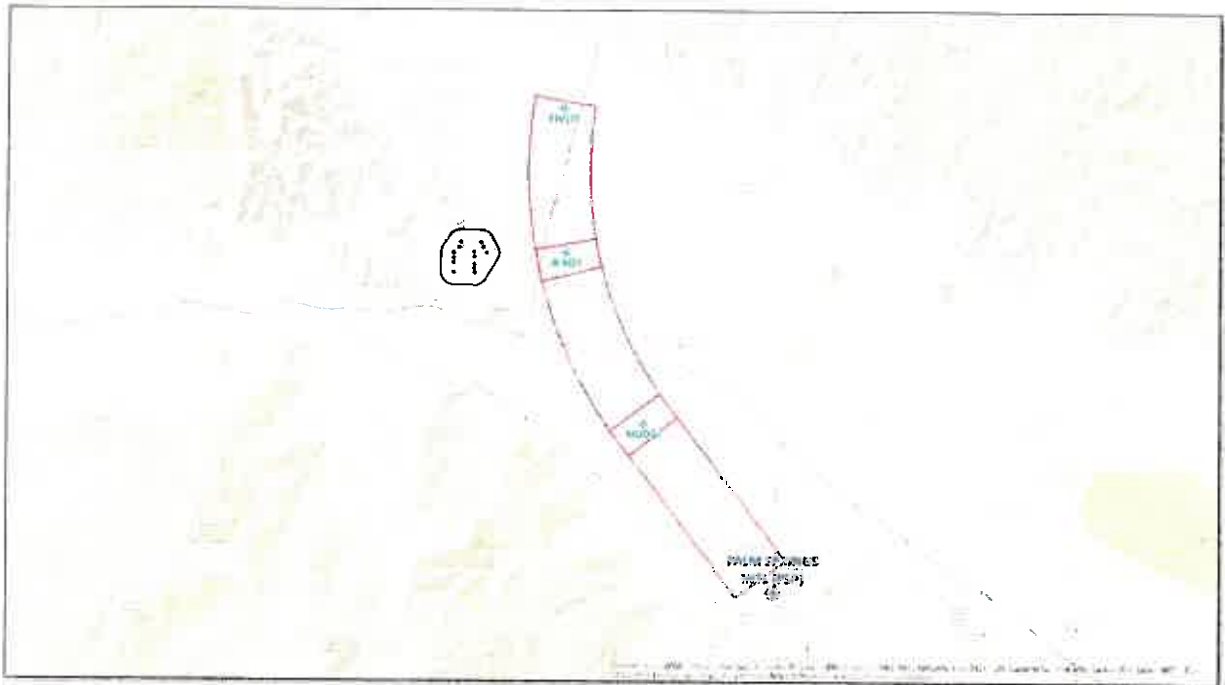


Figure 5: Palm Springs International Airport (PSP) RNAV (RNP) Z Approach to Runway 13R intermediate and final segments

Enroute Airways

Enroute airways provide pilots a means of navigation when flying from airport to airport and are defined by radials between VHF omni-directional ranges (VORs). The FAA publishes minimum altitudes for airways to ensure clearance from obstacles and terrain. The FAA requires that each airway have a minimum of 1,000 feet of obstacle clearance in non-mountainous areas and normally 2,000 feet in mountainous areas.

Proposed structures that exceed enroute airway obstacle clearance surfaces would require an increase to their minimum obstruction clearance altitudes (MOCA) and/or minimum enroute altitudes (MEA). If the FAA determines that this impact would affect a significant volume of operations it could be used as the basis for determination of hazard.

Low altitude enroute airway obstacle clearance surfaces (e.g., *Figure 6*) are in excess of other lower surfaces and should not limit 499 foot AGL wind turbines at any of the proposed locations.



Figure 6: Low altitude enroute chart L-4 with V137 obstacle evaluation area (purple)

Minimum Vectoring/IFR Altitudes

The FAA publishes minimum vectoring altitude (MVA) and minimum instrument flight rules (IFR) altitude charts that define sectors with the lowest altitudes at which air traffic controllers can issue radar vectors to aircraft based on obstacle clearance. The FAA requires that sectors have a minimum of 1,000 feet of obstacle clearance in non-mountainous areas and normally 2,000 feet in mountainous areas.

Proposed structures that exceed minimum vectoring/IFR altitude sector obstacle clearance surfaces would require an increase to the altitudes usable by air traffic control for vectoring aircraft. If the FAA determines that this impact would affect a significant volume of operations, it could result in determinations of hazard.

Minimum vectoring/IFR altitude sector obstacle clearance surfaces (e.g., *Figure 7*) are in excess of other lower surfaces and should not limit 499 foot AGL wind turbines at any of the proposed locations.

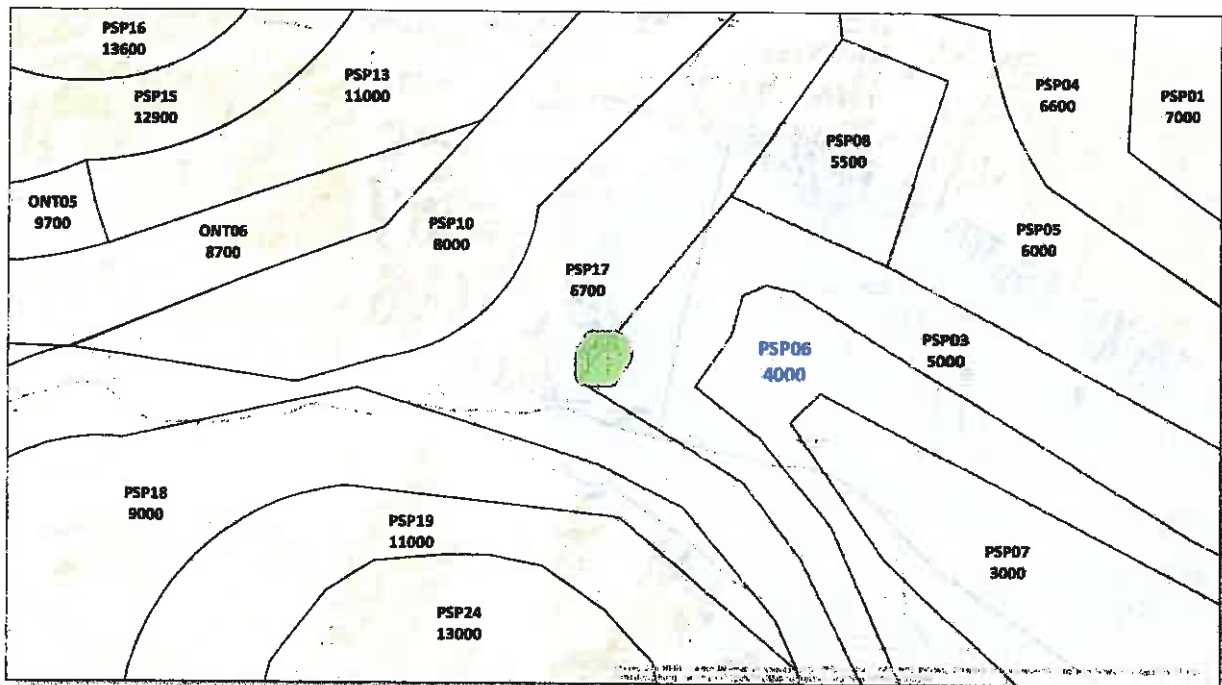


Figure 7: Southern California (SCT) TRACON "FUSION 3" minimum vectoring altitude sectors (black) with Sector PSP06 obstacle evaluation area (hatched blue)

Very High Frequency (VHF) Omni-Directional Range (VOR)

The FAA has established 0.60° (Conventional VOR) and 0.75° (Doppler VOR) screening angles in order to identify proposed structures that may have a negative impact on navigational aids. This surface extends upward and outward from the navigational aid to a distance of up to 8 nautical miles. Proposed wind turbines that exceed this surface may interfere with the services provided by the navigational aid. If the FAA determines this impact to be significant it can be used as the basis for a determination of hazard.

VOR screening surfaces do not overlie the Painted Hills Wind Energy Repowering Project. As a result, it is unlikely that proposed wind turbines would require in depth review by the FAA. In accordance with FAA Technical Operations OEAAA Desk Guide Version 1.2.1, §6.2.1(b)(1), FAA Technical Operations should issue a 'No Objection.'

Long Range and NEXRAD Radar

While Capitol Airspace did not assess for electromagnetic interference on communications, navigational or surveillance systems, the FAA/DOD preliminary screening tool was utilized to determine likely electromagnetic interference on long range and NEXRAD radars.² According to the Long Range Radar tool, the Painted Hills Wind Energy Repowering Project is located in an area designated as 'Green' (left, [Figure 8](#)). The FAA defines this area as follows:

Green: No anticipated impact to Air Defense and Homeland Security radars. Aeronautical study required.

Further, according to the NEXRAD tool, the Painted Hills Wind Energy Repowering Project is located in an area designated as 'Green: No Impact Zone' (right, [Figure 8](#)). The FAA defines this area as follows:

Green: No Impact Zone. Impacts not likely. NOAA will not perform a detailed analysis, but would still like to know about the project.

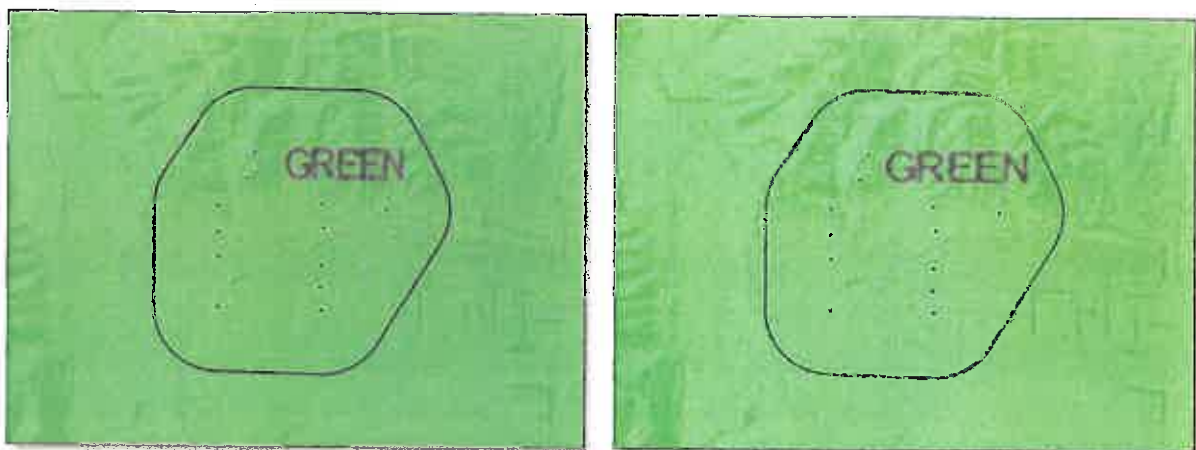


Figure 8: Long range (left) and NEXRAD (right) radar screening tool results

² The preliminary screening tool does not consider turbine height nor does it consider the cumulative impact of existing turbines in proximity to the area studied.

Military Airspace and Training Routes

Since the FAA does not protect for military airspace or training routes, impact on their operations cannot result in a determination of hazard. However, the FAA will notify the military of proposed wind turbines located within these segments of airspace. If the planned development area is located on federal land, impact on military airspace or training routes may result in the denial of permits by the Bureau of Land Management.

Military airspace and training routes do not overlie the Painted Hills Wind Energy Repowering Project. Therefore, these segments of airspace should not result in military objections to proposed wind development.

Conclusion

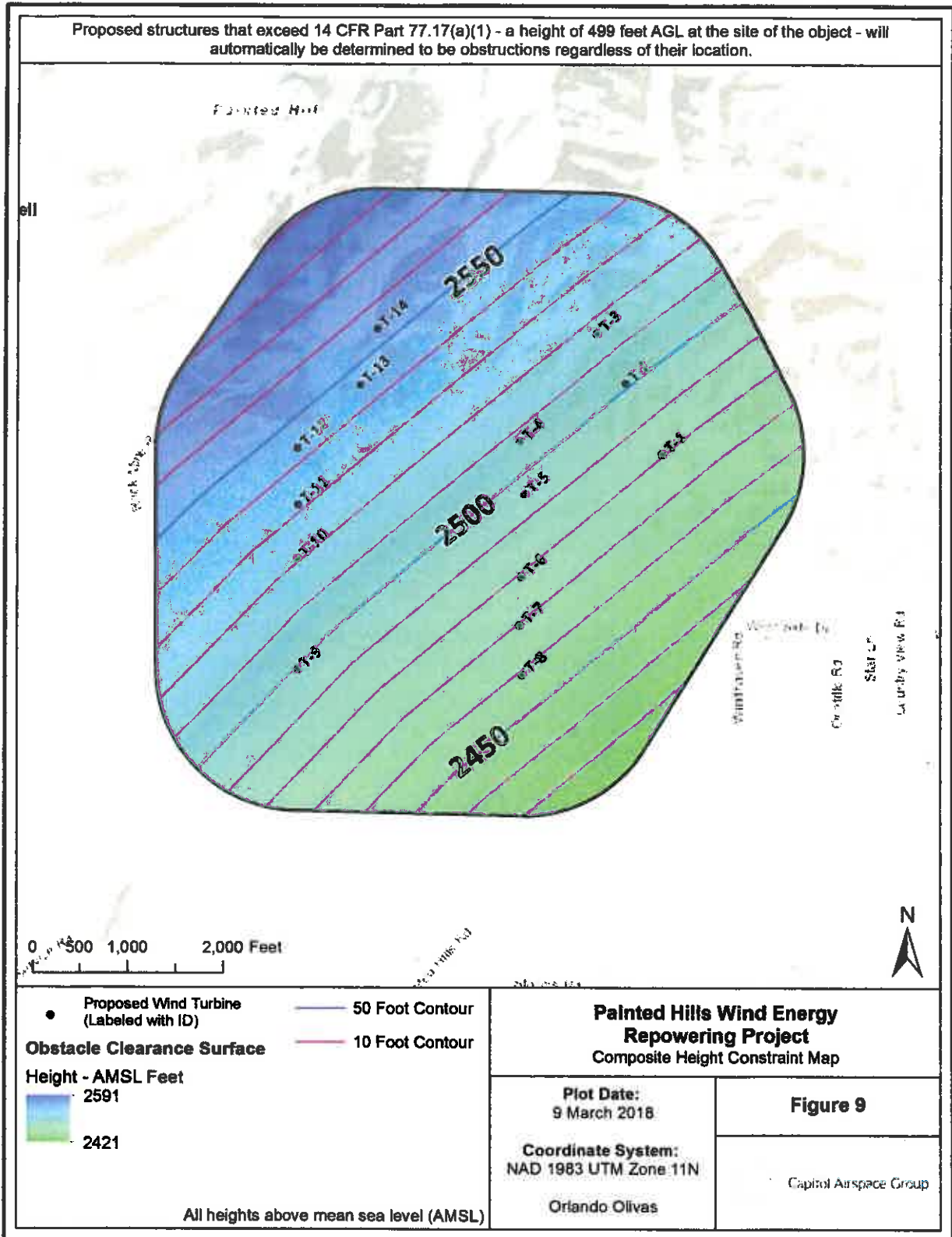
At 499 feet AGL, proposed wind turbines will not exceed 14 CFR Part 77.17(a)(1), 77.17(a)(2), or 77.19/21/23 imaginary surfaces (*Figure 2*). However, proposed wind turbines must remain below obstacle clearance surfaces in order to avoid the likelihood of determinations of hazard.

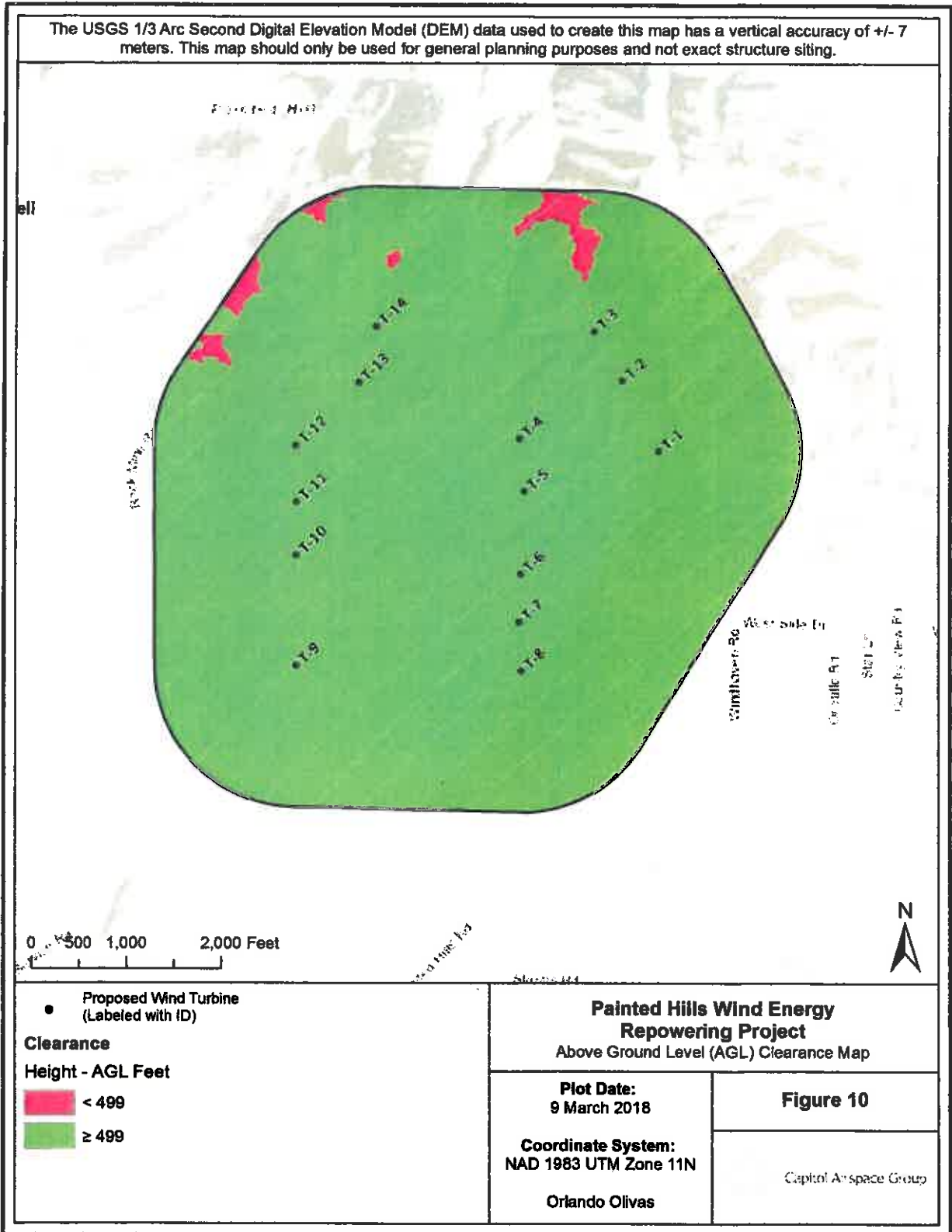
Obstacle clearance surfaces overlying the Painted Hills Wind Energy Repowering Project range from 2,421 to 2,591 feet AMSL (*Figure 9*) and are associated with the Palm Springs International Airport Cathedral One standard instrument departure procedure from Runway 31L (*Figure 4*). Proposed wind turbines that exceed this surface would require an increase to the Runway 31L minimum climb gradient and/or routing. If the FAA determines this impact to constitute a substantial adverse effect, it could be used as the basis for determinations of hazard.

USGS elevation data indicates that this surface could limit wind development on higher terrain in a few small areas within the study area (red areas, *Figure 10*). However, this surface should not limit 499 foot AGL wind turbines at any of the proposed locations (green area, *Figure 10*).

The AGL Clearance Map (*Figure 10*) is based on USGS National Elevation Dataset (NED) 1/3 Arc Second data which has a vertical accuracy of generally +/- 7 meters. Therefore, the AGL Clearance Map should only be used for general planning purposes and not exact wind turbine siting. In order to avoid the likelihood of determinations of hazard, proposed wind turbine heights must adhere to the height constraints depicted in the Composite Map (*Figure 9*).

If you have any questions regarding the findings of this study, please contact *Orlando Olivas* or *Ben Doyle* at (703) 256-2485.





NOTICE OF PUBLIC HEARING

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Thursday from 8:00 a.m. to 5:00 p.m., except Monday, September 3 (Labor Day), and by prescheduled appointment on Friday, September 7, from 9:00 a.m. to 5:00 p.m.

ATTENTION: ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. The County of Riverside may hold hearings on this project and should be contacted on non-ALUC issues.

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon St., 1st Floor Board Chambers
Riverside, California

DATE OF HEARING: September 13, 2018

TIME OF HEARING: 9:30 A.M.

CASE DESCRIPTION:

ZAP1068PS18 – Painted Hills Wind, LLC (Representative: Robert Skaggs) – Riverside County Planning Case Nos. WCS180001 (Commercial WECS Permit) and VAR180003 (Variance). The applicant proposes to decommission and remove approximately 291 existing commercial wind turbines and install 14 new commercial wind turbines with a maximum height of 499 feet with a per turbine energy generating capacity between 2.0 megawatts (MW) and 4.2 MW on 600 acres located northerly of Avenue 16, easterly of Whitewater Canyon Road, westerly of Windhaven Road at the terminus of Painted Hills Road. Also proposed are associated equipment such as a laydown yard, construction of new temporary and permanent internal roads, and new 12-kilovolt (kV) underground/overhead electrical collection lines. A variance is also requested proposing reductions in safety, wind access, and scenic setbacks. (Not located within an Airport Compatibility Zone).

FURTHER INFORMATION: Contact Paul Rull at (951) 955-6893. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Mr. Jay Olivas of the Riverside County Planning Department at (760) 863-7050.



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP1068PS18 DATE SUBMITTED: 7-24-18

APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

Applicant	Painted Hills Wind, LLC	Phone Number 858-764-3720
Mailing Address	11455 El Camino Real, Suite 160 San Diego, CA 92130	Email contact@terra-gen.com
Representative	Robert Skaggs	Phone Number 858-764-3737
Mailing Address	11455 El Camino Real, Suite 160 San Diego, CA 92130	Email rskaggs@terra-gen.com
Property Owner	See Attachment A, Landowners	Phone Number
Mailing Address		Email

LOCAL JURISDICTION AGENCY

Local Agency Name	County of Riverside	Phone Number 760-863-7050
Staff Contact	Jay Olivas	Email jolivas@rivco.org
Mailing Address	77-588 El Duna Court, Suite H Palm Desert, CA 92211	Case Type WECS Permit
Local Agency Project No	WCS180001	<input type="checkbox"/> General Plan / Specific Plan Amendment <input type="checkbox"/> Zoning Ordinance Amendment <input type="checkbox"/> Subdivision Parcel Map / Tentative Tract <input checked="" type="checkbox"/> Use Permit <input type="checkbox"/> Site Plan Review/Plot Plan <input type="checkbox"/> Other

P. Sp. OUTSIDE ALA.

PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address	West of Windhaven Road and North of 16th Avenue		
Assessor's Parcel No.	516-030-004, 516-030-008, 516-030-014, 516-030-015	Gross Parcel Size	600 acres MOL
Subdivision Name		Nearest Airport	
Lot Number		and distance from Airport	Palm Springs Int'l (PSP); Approximately 9 miles

PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe)	See Plans Package (Exhibit A, Site Plan)

Proposed Land Use (describe)	See Attachment B, Project Description		
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)		
For Other Land Uses (See Appendix C)	Hours of Operation	24 hours per day	
	Number of People on Site +/-5	Maximum Number +/-10	
	Method of Calculation	Under the existing condition, between 5-10 employees may be on-site at any given time. Following implementation of the Project, the same number of O&M workers will be needed.	
Height Data	Site Elevation (above mean sea level)	1,380 asml (south) and 2,020 asml (north) ft.	
	Height of buildings or structures (from the ground)	Up to 500 ft.	
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?		<input type="checkbox"/> Yes
			<input checked="" type="checkbox"/> No
	If yes, describe		

A. **NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. **REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.

C. **SUBMISSION PACKAGE:**

- 1. Completed ALUC Application Form
- 1. ALUC fee payment
- 1. Plans Package (24x36 folded) (site plans, floor plans, building elevations, landscaping plans, grading plans, subdivision maps)
- 1. Plans Package (8.5x11) (site plans, floor plans, building elevations, landscaping plans, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
- 1. CD with digital files of the plans (pdf)
- 1. Vicinity Map (8.5x11)
- 1. Detailed project description
- 1. Local jurisdiction project transmittal
- 3. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
- 3. Gummed address labels of all surrounding property owners within a 300 foot radius of the project site (**only required if the project is scheduled for a public hearing Commission meeting**). If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10) with ALUC return address. *

* Projects involving heliports/helicopter landing sites will require additional noticing procedures.

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 2.2 3-5

HEARING DATE: October 11, 2018 (continued from September 13, 2018 due to lack of a quorum)

CASE NUMBER: ZAP1075BD18 - City of Indio

APPROVING JURISDICTION: City of Indio

JURISDICTION CASE NO: Indio General Plan 2040 (City of Indio General Plan Update)

MAJOR ISSUES:

The Bermuda Dunes Airport Influence Area (AIA) extends into the City of Indio. The City includes land within all of the Compatibility Zones (A, B1, B2, C, D, and E), as well as areas outside the AIA. (Areas outside the AIA are not within ALUC's jurisdiction.) To the extent that the designations reflect existing land uses (including projects that have already received their final discretionary approval from the City of Indio), there is no conflict, as ALUC has no jurisdiction over existing land use. The proposed General Plan Land Use Map designates lands within Airport Compatibility Zones B1, B2, C, and D for land use densities and intensities that are not consistent with the 2004 Bermuda Dunes Airport Land Use Compatibility Plan. The proposed General Plan text will require additions and revisions in order to enable a consistency determination. ~~As of the date of preparation of this staff report (August 23, 2018), staff review is ongoing. While we hope to be able to ultimately reach a finding of consistency with the 2004 Bermuda Dunes Airport Land Use Compatibility Plan, subject to certain changes being made to the maps and/or text of the proposed General Plan, at this time, we must recommend a continuance.~~

RECOMMENDATIONS:

As initially submitted, the proposed General Plan Update is inconsistent with the Bermuda Dunes Airport Land Use Compatibility Plan. However, staff would prefer to find a path to consistency. ~~At this time, staff recommends that the Commission open the public hearing, consider testimony, and CONTINUE consideration of this matter to its October 11, 2018 public hearing agenda.~~

~~As of the date of this staff report, the City of Indio has not requested or consented to a continuance. Due to the provisions of the Public Utilities Code, the Commission must render its determination within 60 days of project submittal unless the City agrees to a continuance. In the event that the City is not willing to agree to a continuance, staff would have to~~

~~recommend a finding of inconsistency.~~

The City of Indio did agree to a continuance to the October 11 agenda. However, as of September 19, 2018, ALUC staff has not received any revisions to the maps or text of the proposed General Plan that would bring the Plan into consistency. Therefore, at this time, staff would have to recommend a finding of INCONSISTENCY for the proposed General Plan.

This recommendation is subject to change if the City of Indio, at minimum, agrees to revise the General Plan in accordance with the recommendations offered in the e-mail sent to the City on September 14, 2018. ALUC staff is able to use amenable to using the additional time between the date of this staff report and the hearing date to work out additional details with City staff and would support an additional continuance if requested by the City.

PROJECT DESCRIPTION:

The City of Indio proposes to adopt General Plan 2040, a comprehensive update to the City's General Plan to guide the long-term development of the City and its sphere of influence. The General Plan Update includes the following elements: Land Use and Urban Design, Mobility, Economic Development, Public Health and Equity, Parks and Recreation, Conservation, Community Facilities and Infrastructure, Safety, Noise, and Implementation. (The Housing Element is also a part of the General Plan, but no changes are proposed through this effort.) The City includes land within all Compatibility Zones (A, B1, B2, C, D, and E) of the Bermuda Dunes Airport Influence Area.

PROJECT LOCATION:

All land within the City of Indio, its Sphere of Influence, and unincorporated areas within the County of Riverside that could potentially be annexed into the City. Except for objects 200 feet or greater in height, the jurisdiction of the Airport Land Use Commission is confined to the portions of the City and its Sphere of Influence and Planning Area within the Airport Influence Area of Bermuda Dunes Airport.

AIRPORT LAND USE COMPATIBILITY REFERENCES IN GENERAL PLAN:

The Bermuda Dunes Airport is acknowledged in the Land Use and Urban Design Element, with the map of Compatibility Zones illustrated on Figure 3-3. There is a reference to "use restrictions" within the Airport's "adjacency," but these restrictions are not specified.

Proposed Policy LU-1.6 "Bermuda Dunes Airport Capability [sic]" states as follows: "Indio shall work to achieve consistency between General Plan land use and the Airport Comprehensive Land Use Plan [old name for ALUCP], as is appropriate for the community. Measures may include restrictions on permitted land uses and development criteria, including height, land use type, and intensity."

On page 4-2 of the Mobility Element, it states that "The City will maintain a safe and efficient

system for delivering goods and services, specifying truck routes on City streets, and shall maintain compatibility with the Bermuda Dunes Airport.”

Page 4-26 of the Mobility Element includes the following discussion:

“In addition to the goods movement corridors noted above, the Bermuda Dunes Airport is adjacent to the City of Indio. As such, the airport flight approaches and departures does [sic] influence potential land use within the City to ensure compatibility with the airport and its flight paths. Most of the compatibility requirements related to types of use and building heights within flight paths of the airport. This mobility element ensures that development near the airport remains consistent with the Bermuda Dunes Airport Master Plan.”

Goal ME-5 of the Mobility Element states as follows: “Truck Routes and Airport Compatibility. The City shall maintain a safe and efficient system for delivering goods and services and ensure compatibility with the Bermuda Dunes Airport.” Policy ME-5.2 Airport Compatibility states as follows: “Support the continued use of the Bermuda Dunes Airport through ensuring consistency with the airport master plan.”

The Safety Element lists a number of potential hazards, but not airplane crashes.

The City submitted its proposed new General Plan for formal Airport Land Use Commission review on July 24, 2018. Commissioners should be receiving a CD copy of the proposed General Plan with this staff report.

At first glance, it would appear that much, if not most, of the land within Airport Compatibility Zones B1, B2, C, and D is proposed for designations on the City’s General Plan Land Use Map that are inconsistent or potentially inconsistent with the density and intensity criteria included in the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, as applied to the Bermuda Dunes Airport Influence Area. The General Plan place types are not the conventional land use designation categories, but, pursuant to State law, they must provide information regarding densities and intensities of land use.

Regional Commercial Districts

The Draft General Plan designates land in Airport Compatibility Zone B1 westerly and northerly of Bermuda Dunes Airport as Regional Commercial, which provides for “large-format retail development along with restaurant and commercial service activity”. This designation would apply to all of the properties in the City located northerly of Varner Road and Interstate 10 and westerly of Adams Street. This area is entirely in Compatibility Zone B1, except for the rear portion of the most easterly parcel, which is partially in Compatibility Zone C. The Regional Commercial designation would allow floor-area ratios ranging from 0.35 to 1.0. However, pursuant to the Countywide Policies presently applicable in this area, Compatibility Zone B1 is limited to an average of 25 persons per acre and a maximum single-acre intensity of 50 persons (applicable to parcels two acres or larger that concentrate their usage within a given acre). A retail establishment occupying an entire

acre of land would be evaluated as having an intensity of 726 persons, while a retail establishment occupying 0.35 acre would be evaluated as having an intensity of 254 persons. A restaurant with a dining area occupying 0.2 acre would be evaluated as having an intensity of 581 persons. Therefore, this designation would allow for intensities that are inconsistent with the Compatibility Zone.

The Regional Commercial designation also applies to a large area located easterly of Adams Street, northerly of Varner Road, and southerly of Avenue 40. This area includes land in Compatibility Zones B2, C, and D, as well as B1. ALUC issued a determination of inconsistency for an auto dealership in this area, which was ultimately overruled by the City Council.

Continuing from west to east along Varner Road, the Regional Commercial designation is also proposed for land on both the easterly and westerly sides of Jefferson Street in the vicinity of its intersection with Varner Road. This area is predominantly located in Compatibility Zone D, although the southeastern tip includes a small area in Compatibility Zone B2.

The Regional Commercial designation is also proposed for land located along the northerly side of Varner Road and Interstate 10 on both the easterly and westerly sides of Monroe Street, extending northerly to Avenue 42. The portions of this area westerly of a straight-line extension of Clinton Street are located in Compatibility Zones C and D. (The areas of this designation farther to the east are in Compatibility Zone E, where intensities for retail and restaurant development are not limited.)

The zoning classifications considered to best correlate with the Regional Commercial District land use designation are Regional Commercial and Community Commercial. However, schools, hospitals, and skilled nursing facilities (all of which are prohibited uses in Compatibility Zones B1, B2, and C) and places of worship (a prohibited use in Compatibility Zones B1 and B2) are listed as “permitted uses in any Regional Commercial” zone, as are uses that could accommodate large numbers of persons, such as theaters, hotels, and large retail stores. Additionally, multi-family residential development is listed as permissible, with a conditional use permit.

Uses permitted in any Community Commercial zone include day care centers (a prohibited use in Compatibility Zones B1, B2, and C) and places of worship. Schools, hospitals, skilled nursing facilities, and multi-family residential development are listed as permissible, with a conditional use permit.

Much of the area north and northwest of the airport proposed for the Regional Commercial designation is currently zoned Business Park. Uses permitted in any Business Park zone include schools and day care centers (prohibited uses in Compatibility Zones B1, B2, and C). Hospitals, licensed care facilities, places of worship, and retail stores are listed as permissible, with a conditional use permit.

Mixed Use Neighborhoods

The Draft General Plan designates land in Airport Compatibility Zones B1, B2, C, and D (and a small portion of Compatibility Zone A) as Mixed Use Neighborhood, which provides for “moderate to

higher-intensity neighborhood development that features a variety of multifamily housing choices and commercial uses along major streets.” This designation allows residential densities up to 40 dwelling units per acre and floor area ratios up to 0.5 for nonresidential development. The residential density is acceptable in Compatibility Zone D, but is inconsistent within the other Compatibility Zones. The Northgate Specific Plan is included in this designation. The Specific Plan was found inconsistent by ALUC, but this decision was overruled by the Indio City Council.

This designation is also applied to the fish-shaped area bounded by Indio Boulevard on the south and the rail line and Interstate 10 on the north, easterly of Jefferson Street. A proposed multi-building industrial, office, and retail center with a self-storage component proposed in this area was reviewed by ALUC and found consistent. However, we would recommend that the proposed designation of this area be changed from Mixed Use Neighborhood, which provides for residential use, to Workplace and Employment Center, with an asterisk providing a more limited floor-area ratio than the 1.0 FAR normally allowed in that designation.

An additional Mixed Use Neighborhood designation is proposed for an area southerly of Indio Boulevard, northerly and southerly of Fred Waring Drive and easterly and westerly of Monroe Street. The area northerly of Fred Waring and westerly of Monroe, as well as much of the area southerly of Fred Waring and westerly of Monroe, is located in Compatibility Zone C, where residential densities are limited to one dwelling unit per five acres and nonresidential intensity is limited to an average of 75 persons per acre. The area east of Monroe is in Compatibility Zone D, where the residential densities greater than 5 dwelling units per acre are acceptable. However, the nonresidential FAR allowance of 0.5 would be a concern.

Similar concerns apply to the proposed Mixed Use Neighborhood located easterly of Madison Street, both northerly and southerly of Avenue 42. The portion westerly of a straight-line northerly extension of Clinton Street and southerly of Avenue 42 is located in Compatibility Zones C and D. The portion westerly of a straight-line extension of Clinton Street and northerly of Avenue 42 is located in Compatibility Zone D. (The portion easterly of the straight-line extension of Clinton Street is in Compatibility Zone E, where residential densities are not restricted pursuant to the Compatibility Plan and most nonresidential uses are permitted.)

The zoning classifications considered to best correlate with the Mixed Use Neighborhood designation are Residential High, Neighborhood Commercial, Community Commercial, and Public. The Residential High zone allows residential development, normally at densities of 12 to 15 dwelling units per acre, which would be consistent with Compatibility Zone D, but not with Compatibility Zones B1, B2, and C. However, it would also allow lower density residential development. On the nonresidential side, places of worship, licensed day care centers, and clubs, lodges, and halls are permissible with a conditional use permit.

The Neighborhood Commercial zone is already described as providing for “convenient small scale shopping and personal service uses in close proximity to residential neighborhoods.” Permitted uses include day care centers (a prohibited use in Compatibility Zones B1, B2, and C), as well as restaurants and large retail establishments such as grocery stores over 15,000 square feet in size.

Conditionally permitted uses include hospitals, skilled nursing facilities, schools, and places of worship. Residential uses are not permitted.

The Public Zone allows schools, libraries, and day care centers (all of which are prohibited uses in Compatibility Zones B1, B2, and C) as permitted uses, with hospitals and licensed community care facilities permissible, subject to a conditional use permit.

Workplace and Employment Districts

The Draft General Plan designates land in Airport Compatibility Zones B1, B2, C, and D easterly and northerly of Bermuda Dunes Airport as Workplace and Employment District, which provides “an area for a wide variety of employment-generating activity, including office, industrial and light manufacturing, research and development, and supportive commercial.” Ostensibly, this is the most suitable category of land uses for areas in the vicinity of an airport, as it excludes new residential development. However, this designation provides for floor-area ratio of up to 1.0 for nonresidential development, which for most uses would result in intensities greater than 100 persons per acre. (The exception would be warehouses, which would have a maximum intensity of 86 persons per acre at a FAR of 1.0.)

The zoning classifications considered to best correlate with the Workplace and Employment District designation are Commercial Office, Business Park, Industrial Park, Manufacturing, and Neighborhood Commercial. The list of uses permitted in the Commercial Office zone without a conditional use permit does not include any of the uses prohibited in Compatibility Zones B1, B2, or C. (Those uses – schools, day care centers, places of worship, hospitals, and licensed community care facilities – would require conditional use permits in the Commercial Office zone, as would restaurants, retail stores, and other high-intensity uses. This would allow City staff or ALUC staff an opportunity to evaluate intensity and consistency with the Compatibility Plan.)

The Industrial Park and Manufacturing zones allows schools, and day care centers as permitted uses, along with other uses of potentially high intensity such as restaurants and health clubs. Uses permissible with a conditional use permit include places of worship, licensed community care facilities, retail stores, and clubs, lodges, or halls, and, in the Industrial Park zone, hospitals.

Connected Neighborhoods

The Draft General Plan designates land in Airport Compatibility Zones C and D easterly of Bermuda Dunes Airport as Connected Neighborhoods, which provide “a broad range of housing choices within a walkable neighborhood setting within a short distance of goods and services. Housing types include single-family detached and attached dwellings of various sizes, courtyard apartments and condos, and medium-sized multifamily buildings designed for neighborhood compatibility.” This designation allows residential development at densities up to 20 dwelling units per acre and nonresidential development at floor-area ratios up to 0.5. This designation would not be consistent within Compatibility Zone C, where densities are limited to one dwelling unit per five acres. However, if we exclude fully developed areas from consideration, the only remaining land

designated Connected Neighborhood in Zone C consists of an area on the south side of Indio Boulevard between the channel on the west and Clinton Street on the east.

Proposed Connected Neighborhoods in Zone D include an area located southerly of Northgate, easterly of Jefferson Street, and westerly of Burr Street and an area located easterly of Madison Street, southerly of Sun City Boulevard, and northerly of Avenue 42.

The zoning classifications considered to best correlate with the Connected Neighborhood designation are Residential Medium, Residential High, Residential Low Central Indio, Country Estate and Visitor Serving, and Public.

The Residential Medium zone prohibits commercial uses and requires conditional use permits for places of worship, licensed community care facilities, day care centers, and clubs, lodges, and halls. While there is no penalty for development at lower densities, it does provide for a threshold density of 6 dwelling units per acre, with a maximum density of 8 dwelling units per acre, and this range is acceptable in Compatibility Zone D.

The Residential Low Central Indio zone does prohibit commercial uses and requires conditional use permits for places of worship and licensed day care centers. However, the threshold density is 3.5 dwelling units per acre, with a minimum lot size of 6,600 square feet (8,000 square feet in new subdivisions). Thus, this zone would result in densities below the minimum five dwelling units per acre required in Compatibility Zone D.

The Country Estate and Visitor Serving zone does not provide for any of the nonresidential uses prohibited in Compatibility Zones B1, B2, C, or D, but it is a low density residential zone that would not allow for the minimum five dwelling units per acre. However, the C-E-5 and C-E-10 zones would be considered consistent within Compatibility Zones C and D due to minimum lot sizes of 5 acres or 10 acres.

Suburban Neighborhoods

Some land in Airport Compatibility Zones C and D easterly of Bermuda Dunes Airport is designated as Suburban Neighborhoods, which provide low-intensity neighborhood development for single-family, detached homes. Most of this area is within developed residential tracts. The closest large undeveloped area proposed for this designation is located northerly of Avenue 40 and easterly of Adams Street in Compatibility Zone D.

The zoning classifications considered to best correlate with the Suburban Neighborhoods designation are Residential Low, Residential Medium, Country Estates and Visitor Serving, Equestrian Estates, Country Estates Transition, and Country Estates Indio Ranchos.

Except for Residential Medium, these zones would result in densities below five dwelling units per acre.

The Residential Low zone prohibits commercial uses and requires conditional use permits for places of worship, licensed community care facilities, and day care centers. However, the threshold density is 3.5 dwelling units per acre and an 8,000 square foot minimum lot size, with a maximum density of 4.0 dwelling units per acre and a minimum lot size of 7,200 square feet. Thus, this zone would result in densities below the minimum five dwelling units per acre required in Compatibility Zone D.

The Equestrian Estates zone allows for licensed in home day care centers serving not more than 12 children, and requires conditional use permits for larger day care centers, schools, and places of worship. However, the maximum density is 2.0 dwelling units per acre, with a minimum lot size of 20,000 square feet. Thus, this zone would result in densities below the minimum five dwelling units per acre required in Compatibility Zone D.

The Country Estates Transition zone does not provide for any of the nonresidential uses prohibited in Compatibility Zones B1, B2, C, and D, while allowing for licensed in home day care centers serving not more than 12 children, but the maximum density is 3.0 dwelling units per acre, with a 13,000 square foot minimum lot size. Thus, this zone would result in densities below the minimum five dwelling units per acre required in Compatibility Zone D.

The Country Estates Indio Ranchos zone does not provide for any of the prohibited nonresidential density, but the minimum area per dwelling unit is one-half acre (21,780 square feet). Thus, this zone would result in densities below the minimum five dwelling units per acre required in Compatibility Zone D.

Downtown

ALUC reviewed the City's proposed Indio Downtown/Old Town Specific Plan, covering 117 acres, including 21.87 acres within the Bermuda Dunes Airport Influence Area, last fall. Staff had initially recommended an inconsistency finding, as the Specific Plan's density and intensity standards as proposed could potentially have allowed uses that would have been inconsistent with Compatibility Zone D density and intensity criteria. The matter was resolved when the City revised its document requiring new residential projects in Zone D (the portion of the project westerly of Oasis Street) to maintain a minimum density of at least five dwelling units per acre and specifying that all subsequent nonresidential development and/or change of use proposals within the portion of the Specific Plan in the airport influence area shall be consistent with the intensity criteria of the Bermuda Dunes Airport Land Use Compatibility Plan, including applicable Countywide criteria as may exist at the time of project review.

STATE HANDBOOK RECOMMENDATIONS:

The California Airport Land Use Planning Handbook issued by the State of California Department of Transportation, Division of Aeronautics, includes a Table 5A, General Plan Consistency Checklist, which is "intended to assist local agencies with modifications necessary to make their local plans and other local policies consistent with the ALUCP." While the checklist "is not intended as a state requirement," failure to incorporate most of the items referenced would be a cause for concern.

One of the requirements is that there be no direct conflicts “between proposed new land uses indicated on a general plan land use map and the ALUC land use compatibility criteria.

A second requirement is that criteria indicating the maximum noise exposure for which residential development is normally acceptable “must be made consistent with the equivalent ALUCP criteria.” However, it also states that “a general plan may establish a different limit with respect to aviation-related noise than for noise from other sources,” noting that “this may be appropriate in that aviation-related noise is sometimes judged to be more objectionable than other types of equally loud noises.”

The remaining recommended requirements may be included in either a General Plan or an implementing document such as a Zoning Code. Such document should incorporate ALUCP standards including, but not limited to (as applicable): intensity limits on nonresidential uses; identification of prohibited uses; open land requirements; infill development; height limitations; hazards to flight; buyer awareness measures; and nonconforming uses and reconstruction.

In addition to incorporation of ALUCP compatibility criteria, Table 5A states that “local agency implementing documents must specify the manner in which development proposals will be reviewed for consistency with the compatibility criteria.” This would include: identification of the types of actions that would be required to be submitted for ALUC review; identification of the types of actions potentially subject to ALUC review; procedures that the City would use to evaluate the consistency of other projects with ALUCP compatibility criteria; variance procedures; and enforcement.

A copy of Table 5A is included herewith.

3.5



September 4, 2018

Mr. John Guerin
Principal Planner
Riverside County Airport Land Use Commission
4080 Lemon Street, 14th Floor
Riverside CA 92501

RE: ZAP1075BD18- CITY OF INDIO GENERAL PLAN 2040.

Dear Mr. Guerin,

Following our previous discussion, the City of Indio is in agreement with and accepting of your recommendation to continue the hearing of the City's General Plan 2040 to the October 11, 2018 Airport Land Use Commission meeting. This action will provide additional time necessary to fully consider your comments and revise the draft document to fully comply with the Bermuda Dunes Airport Land Use Compatibility Plan. We sincerely appreciate the time and assistance you have provided and look forward to the October 11 meeting.

Please feel free to contact Leila Namvar at Lnamvar@indio.org or myself, should you have any questions.

Sincerely



Les Johnson,
Development Services Director

Guerin, John

From: Guerin, John
Sent: Friday, September 14, 2018 11:35 AM
To: 'Leila Namvar'
Cc: Housman, Simon; Rull, Paul; Les Johnson
Subject: RE: City of Indio General Plan Update - Recommended Amendments

The Suburban Neighborhood, Connected Neighborhood, Mixed Use Neighborhood, and Neighborhood Commercial designations allow for residential development at urban densities. Such development is not consistent within Compatibility Zones B1, B2, and C. Compatibility Zone D allows urban density residential development at densities of at least five dwelling units per acre net density. We recommend adding a policy stating that new dwelling units shall not be permitted in the portions of these designations within Compatibility Zones B1, B2, and C, except where such development has already received its final discretionary approval (tentative map/use permit/design review) or is subject to a pre-existing development agreement.

As none of the residential designations within Zone D prohibit residential development at higher urban densities, none are completely inconsistent in those areas. However, we recommend adding a policy specifying that new residential development in the portions of these designations within Compatibility Zone D shall have a net density of at least 5 dwelling units per acre.

The District and Center designations allow for a variety of nonresidential land uses, which is appropriate, but the allowable Floor Area Ratios range as high as 2.0 Downtown, 1.0 in the Regional Commercial and Workplace and Employment Districts, and 0.5 in the Mixed Use Neighborhood, Neighborhood Center, and Public and Institutional Districts. There is no reference to the specific land use intensity limitations from the Countywide Policies of the Riverside County Airport Land Use Compatibility Plan (applicable within the Bermuda Dunes Airport Influence Area). Given these intensity limits, the high Floor Area Ratios noted here would not be possible in Airport Compatibility Zones B1, B2, and C (except for warehousing uses), and may not be achievable in Compatibility Zone D. We recommend adding a policy stating that land use intensity of nonresidential structures is limited within Compatibility Zones B1, B2, C, and D and that, within those areas, allowable Floor Area Ratios would be affected by the land use intensity limitations applicable within these zones.

As noted in our staff report, some of the zones listed as correlating with the General Plan place types allow uses that would not be consistent within specified Compatibility Zones. The best way to remedy that situation would be the establishment of an airport overlay to the zoning ordinance. In the absence of amendments to the zoning ordinance, this can be addressed through a General Plan policy specifying the list of prohibited uses by Compatibility Zone.

The General Plan should also include a statement that, pursuant to State law, all general plan amendments, specific plans, specific plan amendments, zone changes, and ordinance amendments affecting land/development potential within the Airport Influence Area (including Citywide amendments) shall be submitted to the Riverside County Airport Land Use Commission for review.

Provided that ALUC is ultimately able to make a determination of consistency, other types of projects would not require official review by ALUC. However, that does not constitute a waiver of the applicability of the density and intensity limitations of the Compatibility Plan. Rather, it means that City staff take on the responsibility of verifying that projects comply with the Compatibility Plan. (See City of Eastvale's zoning ordinance.)

Finally, relative to data, I am still awaiting information as to unrecorded, but tentatively approved. portions of Espana project in Suburban designation and information regarding approval of Northgate multi-family projects in Mixed Use designation.

From: Guerin, John <JGUERIN@RIVCO.ORG>
Sent: Tuesday, August 21, 2018 5:56 PM
To: Leila Namvar <lnamvar@indio.org>
Cc: Housman, Simon <shousman@rivco.org>; Rull, Paul <PRull@RIVCO.ORG>
Subject: RE: City of Indio General Plan Update - Comments, Part 1

Thank you for including reference to the Bermuda Dunes Airport in the proposed General Plan Land Use and Urban Design Element and for including a map of the Compatibility Zones (page 3-10). The text also references the fact that there are associated use restrictions (page 3-9), but errs in tying these to the noise contours. Additionally, it references "Capability Plan" rather than "Compatibility Plan." Also, "CLUP" is an outmoded acronym that applied to the "Comprehensive Land Use Plans" of the previous generation. The correct term now is "Airport Land Use Compatibility Plans" and the correct acronym is "ALUCP(s)."

The General Plan should include sufficient information regarding applicable ALUCPs that would enable residents and landowners to be aware of applicable land use restrictions. In this regard, we recommend that Table 2A from the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan be included in your Land Use Element in order to make the map legend meaningful.

If the City is reluctant to "lock in" the Table 2A restrictions, an alternative that has been found acceptable in the past would be to include a reference to ALUC's website or contact information with the map.

The City includes land in all of the Airport Compatibility Zones of Bermuda Dunes Airport. There is only a small portion in Zone A, where no structures are permitted. Zone E is not subject to density or intensity restrictions, but Zones B1, B2, C, and D have density and intensity restrictions. Land in Zones B1, B2, and C that has not already been committed to residential use (existing uses or projects that have received their final discretionary approval) should be designated for low intensity nonresidential uses.

Zone D in urban areas is designed for densities of at least five dwelling units per acre. In this regard, we support the City's proposal that densities up to 8 dwelling units per acre be permitted in future Suburban Neighborhoods. Maintaining an upper level of 8 avoids the situation where a jurisdiction limits density levels in a manner that results in a property owner in Zone D finding no density that would meet both the density provisions of the jurisdiction and the ALUCP. However, while Suburban Neighborhoods would allow up to 8, Connected Neighborhoods up to 20, and Mixed Use Neighborhoods up to 40 dwelling units per acre, there does not seem to be a minimum density specified.

When the Downtown Specific Plan was under consideration, this was resolved by requiring densities of at least 5 dwelling units per acre in the portion of the Specific Plan within Zone D of the Airport Influence Area (which was the portion westerly of Oasis Street). We would recommend that proposed Table 3-2 include a note stating that densities of new developments in Compatibility Zone D be at least five dwelling units per acre.

With regard to nonresidential intensity, the projected maximum FARs for the Regional Commercial (1.0), Workplace and Employment District (1.0), Mixed Use Neighborhood (0.5), Neighborhood Center (0.5), and Connected Neighborhood (0.25) designations would be expected to result in intensities exceeding ALUCP nonresidential intensity limits (average of 25 persons per acre in Compatibility Zone B1, 75 persons per acre in Compatibility Zone C, and 100 persons per acre in Compatibility Zones B2 and D). The matter is exacerbated by the asterisk statement on the bottom of this table on page 3-16 allowing portions of the large projects to exceed these densities "as long as the total project density does not exceed the allowable density or FAR for the placetype."

Again, a similar situation arose in consideration of the Downtown Specific Plan. We recommended adding language to the text specifying that new nonresidential development and/or change of use proposals within portions in the Airport

Influence Area “shall be consistent with intensity criteria of the Bermuda Dunes Airport Land Use Compatibility Plan, including applicable Countywide criteria as may exist at the time of project review.”

The effect of the Compatibility Zone criteria on the design of policies is not evident. Some of the uses listed as allowed in Connected and Mixed Use Neighborhoods would be inconsistent either by policy (such as the ALUCP’s prohibition of schools and hospitals in Zones A, B1, B2, and C) or as a result of anticipated intensity (such as large places of worship, theaters, or auditoriums in Compatibility Zones B1, B2, C, and D). Additionally, some of the General Plan policies seem to encourage uses such as “flex” and “pop-up” that could result in intensities that go over the limits.

We are in the process of assessing specific geographic areas of conflict and will provide additional comments in that regard. In the meantime, please see the attached excerpt from the California Airport Land Use Planning Handbook prepared by CALTRANS Division of Aeronautics.

From: Leila Namvar [<mailto:lnamvar@indio.org>]
Sent: Tuesday, August 21, 2018 8:38 AM
To: Guerin, John <JGUERIN@RIVCO.ORG>
Subject: Housing Element - City of Indio General Plan Update

John,

The housing element is part of the General Plan. However, it is on a different time cycle and was updated last in 2014. It is required to be updated every eight years.

Please let me know if you have any additional questions.

Thanks,
Leila Namvar
Assistant Planner
760-541-4258

From: Guerin, John <JGUERIN@RIVCO.ORG>
Sent: Wednesday, July 18, 2018 2:39 PM
To: Leila Namvar <lnamvar@indio.org>
Cc: Housman, Simon <shousman@rivco.org>
Subject: RE: City of Indio General Plan Update

Assuming you are planning to submit in the next two weeks for our September hearing, I would avoid the 26th and the afternoons of the 30th and the 1st, if possible. Other than that, any time between 9:00 and 3:30 M-TH would work. If you would like to submit on a Friday (short staff), I would advise an appointment.

In addition to the document, we would appreciate submittal of a Land Use Plan exhibit that is at a sufficient scale to allow us to identify parcels, or at least to be able to identify blocks. An 8 ½ x11 copy would not be sufficiently legible. Thanks.

From: Leila Namvar [<mailto:lnamvar@indio.org>]
Sent: Wednesday, July 18, 2018 2:07 PM
To: Guerin, John <JGUERIN@RIVCO.ORG>
Cc: Housman, Simon <shousman@rivco.org>
Subject: RE: City of Indio General Plan Update

John,

Could you please let me know when would be the best day and time to submit our application for the City's General Plan Update? It would be great, if you email me couple of days and times. Also, I would like to add that the application only includes the draft document and it does not include the EIR.

Thanks,
Leila Namvar
Assistant Planner
760-541-4258

From: Guerin, John <JGUERIN@RIVCO.ORG>
Sent: Monday, July 16, 2018 2:14 PM
To: Leila Namvar <lnamvar@indio.org>
Cc: Housman, Simon <shousman@rivco.org>
Subject: RE: City of Indio General Plan Update

"County of Riverside ALUC"

From: Leila Namvar [<mailto:lnamvar@indio.org>]
Sent: Monday, July 16, 2018 2:12 PM
To: Guerin, John <JGUERIN@RIVCO.ORG>
Cc: Housman, Simon <shousman@rivco.org>
Subject: RE: City of Indio General Plan Update

John,

Does the check need to be payable to the "Airport Land Use Commission"? Please advise.

Thanks,
Leila Namvar

From: Guerin, John <JGUERIN@RIVCO.ORG>
Sent: Tuesday, July 10, 2018 9:30 AM
To: Leila Namvar <lnamvar@indio.org>
Cc: Housman, Simon <shousman@rivco.org>
Subject: RE: City of Indio General Plan Update

No. That information is out of date. Our new application form is on our website, www.rcaluc.org, click Forms. The fee is now \$3,696. This was updated last summer. Yes, please submit in person. You may wish to call in advance, so that you are not coming at a time when we're all in meetings. The submittal process takes approximately 35 minutes.

From: Leila Namvar [<mailto:lnamvar@indio.org>]
Sent: Tuesday, July 10, 2018 8:58 AM
To: Guerin, John <JGUERIN@RIVCO.ORG>
Subject: RE: City of Indio General Plan Update
Importance: High

John,

As you know the City of Indio is updating its General Plan. The final public draft is ready for ALUC review. Please let me know if the attached application and fee (\$3,300.00) is still effective. In addition, I would like to know if you will have time for an in person application submittal. Please advise.

Thanks,
Leila Namvar
Assistant Planner
760-541-4258

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This email is confidential and intended solely for the use of the individual(s) to whom it is addressed. The information contained in this message may be privileged and confidential and protected from disclosure. If you are not the author's intended recipient, be advised that you have received this email in error and that any use, dissemination, forwarding, printing, or copying of this email is strictly prohibited. If you have received this email in error please delete all copies, both electronic and printed, and contact the author immediately.

County of Riverside California

Guerin, John

From: Guerin, John
Sent: Friday, September 07, 2018 4:09 PM
To: 'Leila Namvar'
Cc: Housman, Simon; Rull, Paul; 'Les Johnson'
Subject: RE: City of Indio General Plan Update - Parcel-Specific Table (Work in Progress)
Attachments: City of Indio Zones.xlsx

Here is the latest version of the parcel-specific table, inclusive of all properties wholly or partially within Compatibility Zone C.

From: Leila Namvar [<mailto:lnamvar@indio.org>]
Sent: Wednesday, August 22, 2018 9:47 AM
To: Guerin, John <JGUERIN@RIVCO.ORG>
Cc: Housman, Simon <shousman@rivco.org>; Rull, Paul <PRull@RIVCO.ORG>; Les Johnson <ljohnson@indio.org>
Subject: RE: City of Indio General Plan Update - Comments, Part 1

Thank you John. I will share this information with our consultant team and we will contact you, if there is any questions. Looking forward to receive your additional comments.

Thanks,
Leila Namvar
Assistant Planner
760-541-4258

From: Guerin, John <JGUERIN@RIVCO.ORG>
Sent: Tuesday, August 21, 2018 5:56 PM
To: Leila Namvar <lnamvar@indio.org>
Cc: Housman, Simon <shousman@rivco.org>; Rull, Paul <PRull@RIVCO.ORG>
Subject: RE: City of Indio General Plan Update - Comments, Part 1

Thank you for including reference to the Bermuda Dunes Airport in the proposed General Plan Land Use and Urban Design Element and for including a map of the Compatibility Zones (page 3-10). The text also references the fact that there are associated use restrictions (page 3-9), but errs in tying these to the noise contours. Additionally, it references "Capability Plan" rather than "Compatibility Plan." Also, "CLUP" is an outmoded acronym that applied to the "Comprehensive Land Use Plans" of the previous generation. The correct term now is "Airport Land Use Compatibility Plans" and the correct acronym is "ALUCP(s)."

The General Plan should include sufficient information regarding applicable ALUCPs that would enable residents and landowners to be aware of applicable land use restrictions. In this regard, we recommend that Table 2A from the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan be included in your Land Use Element in order to make the map legend meaningful.

If the City is reluctant to "lock in" the Table 2A restrictions, an alternative that has been found acceptable in the past would be to include a reference to ALUC's website or contact information with the map.

The City includes land in all of the Airport Compatibility Zones of Bermuda Dunes Airport. There is only a small portion in Zone A, where no structures are permitted. Zone E is not subject to density or intensity restrictions, but Zones B1, B2, C, and D have density and intensity restrictions. Land in Zones B1, B2, and C that has not already been committed to

residential use (existing uses or projects that have received their final discretionary approval) should be designated for low intensity nonresidential uses.

Zone D in urban areas is designed for densities of at least five dwelling units per acre. In this regard, we support the City's proposal that densities up to 8 dwelling units per acre be permitted in future Suburban Neighborhoods. Maintaining an upper level of 8 avoids the situation where a jurisdiction limits density levels in a manner that results in a property owner in Zone D finding no density that would meet both the density provisions of the jurisdiction and the ALUCP. However, while Suburban Neighborhoods would allow up to 8, Connected Neighborhoods up to 20, and Mixed Use Neighborhoods up to 40 dwelling units per acre, there does not seem to be a minimum density specified.

When the Downtown Specific Plan was under consideration, this was resolved by requiring densities of at least 5 dwelling units per acre in the portion of the Specific Plan within Zone D of the Airport Influence Area (which was the portion westerly of Oasis Street). We would recommend that proposed Table 3-2 include a note stating that densities of new developments in Compatibility Zone D be at least five dwelling units per acre.

With regard to nonresidential intensity, the projected maximum FARs for the Regional Commercial (1.0), Workplace and Employment District (1.0), Mixed Use Neighborhood (0.5), Neighborhood Center (0.5), and Connected Neighborhood (0.25) designations would be expected to result in intensities exceeding ALUCP nonresidential intensity limits (average of 25 persons per acre in Compatibility Zone B1, 75 persons per acre in Compatibility Zone C, and 100 persons per acre in Compatibility Zones B2 and D). The matter is exacerbated by the asterisk statement on the bottom of this table on page 3-16 allowing portions of the large projects to exceed these densities "as long as the total project density does not exceed the allowable density or FAR for the placetype."

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The effect of the Compatibility Zone criteria on the design of policies is not evident. Some of the uses listed as allowed in Connected and Mixed Use Neighborhoods would be inconsistent either by policy (such as the ALUCP's prohibition of schools and hospitals in Zones A, B1, B2, and C) or as a result of anticipated intensity (such as large places of worship, theaters, or auditoriums in Compatibility Zones B1, B2, C, and D). Additionally, some of the General Plan policies seem to encourage uses such as "flex" and "pop-up" that could result in intensities that go over the limits.

We are in the process of assessing specific geographic areas of conflict and will provide additional comments in that regard. In the meantime, please see the attached excerpt from the California Airport Land Use Planning Handbook prepared by CALTRANS Division of Aeronautics.

From: Leila Namvar [<mailto:lnamvar@indio.org>]
Sent: Tuesday, August 21, 2018 8:38 AM
To: Guerin, John <JGUERIN@RIVCO.ORG>
Subject: Housing Element - City of Indio General Plan Update

John,

The housing element is part of the General Plan. However, it is on a different time cycle and was updated last in 2014. It is required to be updated every eight years.

Please let me know if you have any additional questions.

Thanks,
Leila Namvar

Assessor Parcel #	ALUCP Zones	Parcel Size	Proposed General Plan	Existing Zoning	Public?	Homes?
606-060-001	B1	0.9 (ROW)		(ROW)	Railroad	
606-060-002	B1	8.7	Mixed Use Ngrbrhd.	IP		
606-060-009	B1	0.4 (ROW)		(ROW)	City	
606-060-015	B1 & C	3	Mixed Use Ngrbrhd.	PMP		
606-060-020	C & B1	0.82	Mixed Use Ngrbrhd.	PMP		
606-060-021	B1 & C	1.09	Mixed Use Ngrbrhd.	PMP		
606-060-022	B1	3.01	Mixed Use Ngrbrhd.	PMP		
606-060-023	D	2.67	Mixed Use Ngrbrhd.	PMP		
606-060-024	C & B1	2.58	Mixed Use Ngrbrhd.	PMP		
606-060-028	B1 & C	4.48	Mixed Use Ngrbrhd.	PMP		
606-060-029	B1	4.5	Mixed Use Ngrbrhd.	PMP		
606-060-030	D	0.91	Mixed Use Ngrbrhd.	PMP		
606-060-032	D & C	13.04	Mixed Use Ngrbrhd.	PMP		
606-060-034	D & C	0.82	Mixed Use Ngrbrhd.	PMP		
606-060-036	C & D	1.9	Mixed Use Ngrbrhd.	PMP		
606-070-015	D & C	1.51	Parks and Open Space	RL	City	
606-080-001	B1	6.73	Workplace Emp. Ctr.	MU (SP)		
606-080-002	B1	3.54 (ROW)		(ROW)	Railroad	
606-080-003	B1	0.02 (ROW)		(ROW)	State CA	
606-080-004	B1	0.15	Mixed Use Ngrbrhd.	IP		
606-080-005	B1	4.8	Mixed Use Ngrbrhd.	IP		
606-080-006	B1 & C	6.53	Workplace Emp. Ctr.	BP		
606-080-007	B1	1.08	Workplace Emp. Ctr.	BP		
606-080-009	B1, C, & D	18.17	Workplace Emp. Ctr.	BP		
606-080-010	B1	2.11 (ROW)		(ROW)	City	
606-080-011	B1, C, & D	18.38	Workplace Emp. Ctr.	BP		
606-080-012	D	1	Workplace Emp. Ctr.	BP	CVWD	
606-091-001	C, B1, & D	9.1	Workplace Emp. Ctr.	MU (SP)		
606-091-002	B1, or B1 & C	7.17	Workplace Emp. Ctr.	MU (SP)		
606-091-003	B1	0.48	Workplace Emp. Ctr.	MU (SP)		
606-091-004	B1	7.89	Public /Institutional	MU (SP)	IID Imper	
606-091-005	B1	0.57	Public /Institutional	MU (SP)	IID Imper	
606-091-006	B1	1.63	Workplace Emp. Ctr.	MU (SP)		

606-091-007	B1 & C	3.54 (ROW)	(ROW)	Railroad
606-091-008	B1 & C	3.08 Parks and Open Space	AAC Canal	USA
606-091-009	B1 & C	2.02 (ROW)	(ROW)	City
606-092-001	C & B1	3.76 Workplace Emp. Ctr.	IP	
606-092-002	B1 & C	3.3 Parks and Open Space	AAC Canal	USA
606-092-005	B1	6.5 Workplace Emp. Ctr.	IP	
606-092-007	B1	0.97 Workplace Emp. Ctr.	IP	
606-092-010	B1	0.61 Workplace Emp. Ctr.	IP	
606-092-011	B1	0.6 Workplace Emp. Ctr.	IP	
606-092-012	B1 & C	0.63 Workplace Emp. Ctr.	IP	
606-092-013	C & B1	4.49 Workplace Emp. Ctr.	IP	
606-092-016	C, or C & B1	3.54 Workplace Emp. Ctr.	IP	
606-092-017	B1	1.24 Workplace Emp. Ctr.	IP	
606-092-019	B1	0.43 Workplace Emp. Ctr.	IP	
606-092-020	B1	0.47 Workplace Emp. Ctr.	IP	
606-092-021	B1	0.33 Workplace Emp. Ctr.	IP	
606-092-022	B1	0.28 Workplace Emp. Ctr.	IP	
606-092-023	B1, or B1 & C	0.28 Workplace Emp. Ctr.	IP	
606-092-024	B1 & C	0.33 Workplace Emp. Ctr.	IP	
606-092-025	B1	0.38 Workplace Emp. Ctr.	IP	
606-092-026	B1	0.38 Workplace Emp. Ctr.	IP	
606-130-010	D & C	1.36 Parks and Open Space	AAC Canal	USA
606-130-063	D, or D & C	0.58 Suburban Residential	RL	
606-130-065	D & C	7.68 Suburban Residential	RL	
606-140-007	D & C	4.25 Parks and Open Space	AAC Canal	USA
606-650-001	B2, D, & B1	9.98 Mixed Use Ngrbrhd.	PMP	
606-650-002	D	9.9 Mixed Use Ngrbrhd.	PMP	
606-650-004	B1 & A	5.45 Mixed Use Ngrbrhd.	PMP	
606-650-005	B1, or B1 & C	1.03 Mixed Use Ngrbrhd.	PMP	
606-650-006	B1	0.59 Mixed Use Ngrbrhd.	PMP	
606-650-008	D	0.74 Mixed Use Ngrbrhd.	PMP	
606-650-012	B1 & A	3.38 Mixed Use Ngrbrhd.	PMP	
606-650-013	C, D, B2, & B1	5.96 Mixed Use Ngrbrhd.	PMP	
606-650-015	D & B2	2.7 Mixed Use Ngrbrhd.	PMP	

606-650-017	B1 & B2	0.86 Mixed Use Ngrhd.	PMP	
607-031-031	B1	3.36 Regional Commercial	BP	
607-031-032	B1	4.13 Regional Commercial	BP	
607-031-033	B1	4.31 Regional Commercial	BP	
607-031-034	B1 & C	5.27 Regional Commercial	BP	
607-031-035	B1 & C	6.62 Regional Commercial	BP	
607-032-009	B1	12.74 (ROW)	(ROW)	Railroad
607-230-002	D, B1, & B2	17.94 Regional Commercial	BP	
607-230-003	D	0.43 Regional Commercial	BP	Home
607-230-004	D	3.64 Regional Commercial	BP	Home
607-230-005	D	0.52 Regional Commercial	BP	2 Homes
607-230-006	D	0.53 Regional Commercial	BP	
607-230-007	D	5.37 Regional Commercial	BP	
607-230-008	D	0.89 Regional Commercial	BP	
607-230-009	D & B2	0.82 Regional Commercial	BP	
607-230-010	D	0.22 Regional Commercial	BP	
607-230-011	D	0.22 Regional Commercial	BP	
607-230-013	B2	0.28 Regional Commercial	BP	
607-230-014	B2 & D	0.41 Regional Commercial	BP	3 Homes
607-230-019	D	1.43 Workplace Emp. Ctr.	BP	Home
607-230-020	D	4.91 Workplace Emp. Ctr.	BP	
607-230-021	D	1.14 Workplace Emp. Ctr.	BP	Home
607-230-022	D	0.57 Workplace Emp. Ctr.	BP	Home
607-230-023	D	0.09 Workplace Emp. Ctr.	BP	
607-230-030	B1	0.45 (ROW)	(ROW)	Railroad
607-230-035	B1 & D	4.34 Regional Commercial	BP	
607-230-036	B1	4 Regional Commercial	BP	
607-230-042	B1, D, & C	3.28 Regional Commercial	BP	
607-230-045	D	0.84 Workplace Emp. Ctr.	BP	
607-230-046	D	0.5 Workplace Emp. Ctr.	BP	CVWD
607-230-047	B2	0.14 Regional Commercial	BP	
607-230-048	D & B2	1.61 Regional Commercial	BP	
607-230-050	D, C, & B1	2.84 Regional Commercial	BP	

607-251-002	B2	0.13 Regional Commercial	BP		
607-251-004	B2	0.21 Regional Commercial	BP		Home
607-251-005	B2	0.21 Regional Commercial	BP		Home
607-251-006	B2	0.22 Regional Commercial	BP		Home
607-251-007	B2	0.2 Regional Commercial	BP	City	Home
607-251-008	B2	0.29 Regional Commercial	BP	City	
607-251-015	B1 & B2	13.23 (ROW)	(ROW)	Railroad	
607-251-016	B1 & B2	0.74 Regional Commercial	BP		
607-251-017	B1	0.02 Regional Commercial	BP		

607-260-057	D	0.44 Regional Commercial	(ROW)	Caltrans	
607-260-058	D	4.12 Regional Commercial	CC	City	
607-260-059	D	0.22 Regional Commercial	CC		
607-260-060	D	0.44 Regional Commercial	CC		
607-352-001	B2	5.82 (ROW)	(ROW)	Railroad	
607-352-002	B2	4.85 (ROW)	(ROW)	Caltrans	
608-080-008	E & C	6.38 Mixed Use Ngrhd.	RL		Apts.
608-080-017	C & E	8.98 Mixed Use Ngrhd.	CC	County	
608-080-021	C	0.27 Mixed Use Ngrhd.	CC		
608-080-024	C	0.81 Mixed Use Ngrhd.	CC		
608-080-025	C	0.23 Mixed Use Ngrhd.	CC		
608-080-029	C	1.97 Mixed Use Ngrhd.	CC	County	
608-080-030	C	0.68 Mixed Use Ngrhd.	CC		
608-080-032	C & E	8.7 Mixed Use Ngrhd.	CC	County	
610-020-001	D & C	61 Mixed Use Ngrhd.	MU (SP)		
610-020-006	C	14.87 Parks and Open Space	OS	USA	
610-020-007	C	4.04 Parks and Open Space	WSC	CVWD	
610-020-008	C	0.91 Parks and Open Space	WSC	CVWD	
610-020-010	E & C	15.8 Regional Commercial	MU (DA)		
610-020-012	C	3.26 Regional Commercial	MU (SP)		
610-020-013	C & D	16.13 Regional Commercial	MU (SP)		
610-020-014	C	5.61 Parks and Open Space	OS/(ROW)	Railroad	
610-020-015	E & C	34.43 Parks and Open Space	WSC	CVWD	
610-020-016	C, B1, & D	20.56 Parks and Open Space	WSC	USA	
610-020-017	C	1.42 Workplace Emp. Ctr.	IP		
610-020-018	C	1.05 Workplace Emp. Ctr.	IP		
610-020-027	C	1.87 Workplace Emp. Ctr.	IP		
610-020-028	C	0.59 Workplace Emp. Ctr.	IP		
610-020-029	C	0.52 Workplace Emp. Ctr.	IP		
610-020-030	C	0.51 Workplace Emp. Ctr.	IP		
610-020-031	C	0.51 Workplace Emp. Ctr.	IP		
610-020-032	C	0.53 Workplace Emp. Ctr.	IP		

610-020-033	C		0.14 Workplace Emp. Ctr.	IP		
610-030-001	C		5.14 Workplace Emp. Ctr.	IP		
610-030-003	C		2.89 Workplace Emp. Ctr.	IP		Home
610-030-007	C		0.62 Workplace Emp. Ctr.	IP		
610-030-008	C		0.62 Workplace Emp. Ctr.	IP		
610-030-009	C		1.2 Workplace Emp. Ctr.	IP		
610-030-010	C		1.2 Workplace Emp. Ctr.	IP		
610-030-013	C		1.88 (ROW)	OS		Railroad
610-030-014	C		1.54 Parks and Open Space	OS		USA
610-030-015	C	?	Parks and Open Space	M		Vector C
610-030-016	C		1.94 (ROW)	P		Railroad
610-030-017	C		1.26 (ROW)	P		Railroad
610-030-018	C		2.96 Connected Residential	RH		Church
610-030-020	C		4.91 Parks and Open Space	WSC		CVWD
610-030-021	C		13.14 Parks and Open Space	WSC		CVWD
610-030-027	C		2.4 Workplace Emp. Ctr.	IP		
610-030-028	C		5.53 Workplace Emp. Ctr.	IP		
610-030-029	C		0.1 Workplace Emp. Ctr.	IP		
610-030-030	C		0.12 Workplace Emp. Ctr.	IP		
610-030-031	C		0.12 Workplace Emp. Ctr.	IP		
610-030-032	C		0.12 Workplace Emp. Ctr.	IP		
610-030-033	C		0.12 Workplace Emp. Ctr.	IP		
610-030-034	C		0.12 Workplace Emp. Ctr.	IP		
610-030-035	C		0.12 Workplace Emp. Ctr.	IP		
610-030-036	C		0.1 Workplace Emp. Ctr.	IP		
610-030-037	C		1.89 Workplace Emp. Ctr.	IP		
610-040-002	D & C		6.57 Parks and Open Space	WSC		CVWD
610-070-001	C & E		4.48 Parks and Open Space	WSC		CVWD
610-070-002	C & E		23.92 Workplace Emp. Ctr.	M		Vector C
610-070-012	E & C		1.49 Workplace Emp. Ctr.	M		
610-070-013	C & E		1.49 Workplace Emp. Ctr.	M		
610-070-014	E & C		1.08 Workplace Emp. Ctr.	M		
610-070-019	E & C		2.72 Neighborhood Cmrl.	CC		
610-070-020	E & C		0.6 Workplace Emp. Ctr.	M		

610-070-021	C, or C & E	0.6 Workplace Emp. Ctr.	M		
610-070-022	C	0.82 Workplace Emp. Ctr.	M		
610-070-023	C & E	1.24 Workplace Emp. Ctr.	M		
610-070-024	C	1.02 Workplace Emp. Ctr.	M		
610-070-025	E & C	1.37 Workplace Emp. Ctr.	M	Vector C	
610-070-026	C	0.74 Workplace Emp. Ctr.	M		
610-070-027	C	0.99 Workplace Emp. Ctr.	M		
610-070-029	C	0.5 Workplace Emp. Ctr.	M		
610-070-030	C	1.24 Workplace Emp. Ctr.	M		
610-070-031	C	1.43 Workplace Emp. Ctr.	M		
610-070-032	C	1.52 Workplace Emp. Ctr.	M		
610-070-033	C	0.95 Workplace Emp. Ctr.	M		
610-070-034	C	0.52 Workplace Emp. Ctr.	M		
610-070-035	C	2.46 (ROW)	P	Railroad	
610-253-012	C	0.7 Suburban Residential	RL		Home
610-261-023	C	3.02 Suburban Residential	RL		
610-261-024	C & E	1.07 Suburban Residential	RL		Home
610-280-001	C	5.35 Mixed Use Ngrhd.	CC		
610-280-002	C	3.67 Mixed Use Ngrhd.	CC		
610-280-003	C	1.36 Mixed Use Ngrhd.	CC		
610-280-004	C	5.53 Mixed Use Ngrhd.	CC		Hotel
610-280-005	C	9.56 Connected Residential	RM-MHPD		MH PK
610-280-006	C	2.12 Mixed Use Ngrhd.	CC		
610-280-008	C & E	16.72 Connected Residential	RM-MHPD		MH PK
610-280-009	C	4.68 (ROW)	P	Railroad	
610-280-010	C	1.59 Workplace Emp. Ctr.	M		
610-280-011	C	2.08 Workplace Emp. Ctr.	M		
610-280-012	C	? Workplace Emp. Ctr.	CC	IID	
610-280-013	C	3.45 Workplace Emp. Ctr.	M		
610-280-014	C	0.02 Workplace Emp. Ctr.	M		
610-280-015	C	3.38 Workplace Emp. Ctr.	M		
610-280-016	C	0.52 Workplace Emp. Ctr.	M		
610-280-021	C	1.87 Workplace Emp. Ctr.	M		
610-280-022	C	0.87 Workplace Emp. Ctr.	M		

610-280-023	E & C		6 Workplace Emp. Ctr.	M		
610-280-024	C		2.63 Mixed Use Ngrbrhd.	CC		
610-280-025	C		0.02 Mixed Use Ngrbrhd.	CC		
691-070-021	D & C		14.86 Suburban Residential	RL		
691-190-004	D & C		1.01 Suburban Residential	PMP		
691-190-006	D & C		3.46 Public/Institutional	AAC canal	USA	
691-190-007	D & C		7.9 Workplace Emp. Ctr.	AAC canal	CVWD	
691-190-009	B1		12.14 (ROW)	(ROW)	Railroad	
691-190-011	C		0.45 Workplace Emp. Ctr.	PMP/LMM		
691-190-012	C		0.59 Suburban Residential	PMP	CVWD	
691-190-013	D & C		4.08 Parks and Open Space	PMP	HOA	
691-190-014	D		0.55 Suburban Residential	PMP	CVWD	
691-190-016	C & D		8.37 Workplace Emp. Ctr.	PMP/LMM		
691-190-017	D & C		4.95 Workplace Emp. Ctr.	MU (SP)/LMM		
691-190-018	C & B1		0.67 Workplace Emp. Ctr.	CO-PMP/LMM		
691-190-019	D & C		0.28 Public/Institutional	MU (SP)/LMM		
691-190-020	D & C		4.34 Parks and Open Space	AAC canal	CVWD	
691-190-022	B1 & C		0.2 Workplace Emp. Ctr.	MU (SP)/LMM		
691-190-023	B1 & C		18.2 Workplace Emp. Ctr.	MU (SP)/LMM		
691-190-024	C		0.31 Public/Institutional	MU (SP)/LMM		
691-190-025	C & D		5.21 Workplace Emp. Ctr.	MU (SP)/LMM		
691-190-026	D & C		11.01 Workplace Emp. Ctr.	MU (SP)		
691-190-029	D, B2, & C		40.78 Workplace Emp. Ctr.	PMP		
691-190-031	B1 & C		12.03 Workplace Emp. Ctr.	CO-PMP/LMM		
691-190-035	B1 & A		3.87 Mixed Use Ngrbrhd.	IP		
691-380-073	C		0.23 Suburban Residential	PMP		Home
691-380-091	D & C		0.46 Suburban Residential	PMP	HOA	
691-380-092	D & C		2.51 Suburban Residential	PMP	HOA	
748-280-014	B1	?	Regional Commercial	BP		
748-280-015	B1		1.54 Regional Commercial	BP		
748-280-018	B1		1.37 Regional Commercial	BP		
748-380-011	B1		0.29 Regional Commercial	BP	Caltrans	
748-420-075	B1		0.04 Regional Commercial	BP		

TABLE 5A: GENERAL PLAN CONSISTENCY CHECKLIST

For additional guidance see:

COMPATIBILITY CRITERIA

This checklist is intended to assist local agencies with modifications necessary to make their local plans and other local policies consistent with the ALUCP. It is also designed to facilitate ALUC reviews of these local plans and policies. The list will need to be modified to reflect the policies of each individual ALUC and is not intended as a state requirement.

General Plan Document

The following items typically appear directly in a general plan document. Amendment of the general plan will be required if there are any conflicts with the ALUCP

Page 6-17

- **Land Use Map**—No direct conflicts should exist between proposed new land uses indicated on a general plan land use map and the ALUC land use compatibility criteria.
 - Residential densities (dwelling units per acre) should not exceed the set limits.
 - Proposed nonresidential development needs to be assessed with respect to applicable intensity limits (see below).
 - No new land uses of a type listed as specifically prohibited should be shown within affected areas.

Pages 3-8

- **Noise Element**—General plan noise elements typically include criteria indicating the maximum noise exposure for which residential development is normally acceptable. This limit must be made consistent with the equivalent ALUCP criteria. Note, however, that a general plan may establish a different limit with respect to aviation-related noise than for noise from other sources (this may be appropriate in that aviation-related noise is sometimes judged to be more objectionable than other types of equally loud noises).

Zoning or Other Policy Documents

The following items need to be reflected either in the general plan or in a separate policy document such as a combining zone ordinance. If a separate policy document is adopted, modification of the general plan to achieve consistency with the ALUCP may not be required. Modifications would normally be needed only to eliminate any conflicting language which may be present and to make reference to the separate policy document.

Page 4-26, Appendix G

- **Intensity Limitations on Nonresidential Uses**—ALUCPs may establish limits on the usage intensities of commercial, industrial, and other nonresidential land uses. This can be done by duplication of the performance-oriented criteria—specifically, the number of people per acre—indicated in the ALUCP. Alternatively, ALUCs may create a detailed list of land uses which are allowable and/or not allowable within each compatibility zone. For certain land uses, such a list may need to include limits on building sizes, floor area ratios, habitable floors, and/or other design parameters which are equivalent to the usage intensity criteria.

Pages 3-11, 4-29, Figures 4B - G

- **Identification of Prohibited Uses**—ALUCPs may prohibit schools, day care centers, assisted living centers, hospitals, and other uses within a majority of an airport's influence area. The facilities often are permitted or conditionally permitted uses within many commercial or industrial land use designations.

Page 4-31

- **Open Land Requirements**—ALUCP requirements, if any, for assuring that a minimum amount of open land is preserved in the airport vicinity must be reflected in local policies. Normally, the locations which are intended to be maintained as open land would be identified on a map with the total acreage within each compatibility zone indicated. If some of the area included as open land is private property, then policies must be established which assure that the open land will continue to exist as the property develops. Policies specifying the required characteristics of eligible open land should also be established.

Page 3-56, 4-18, 4-42

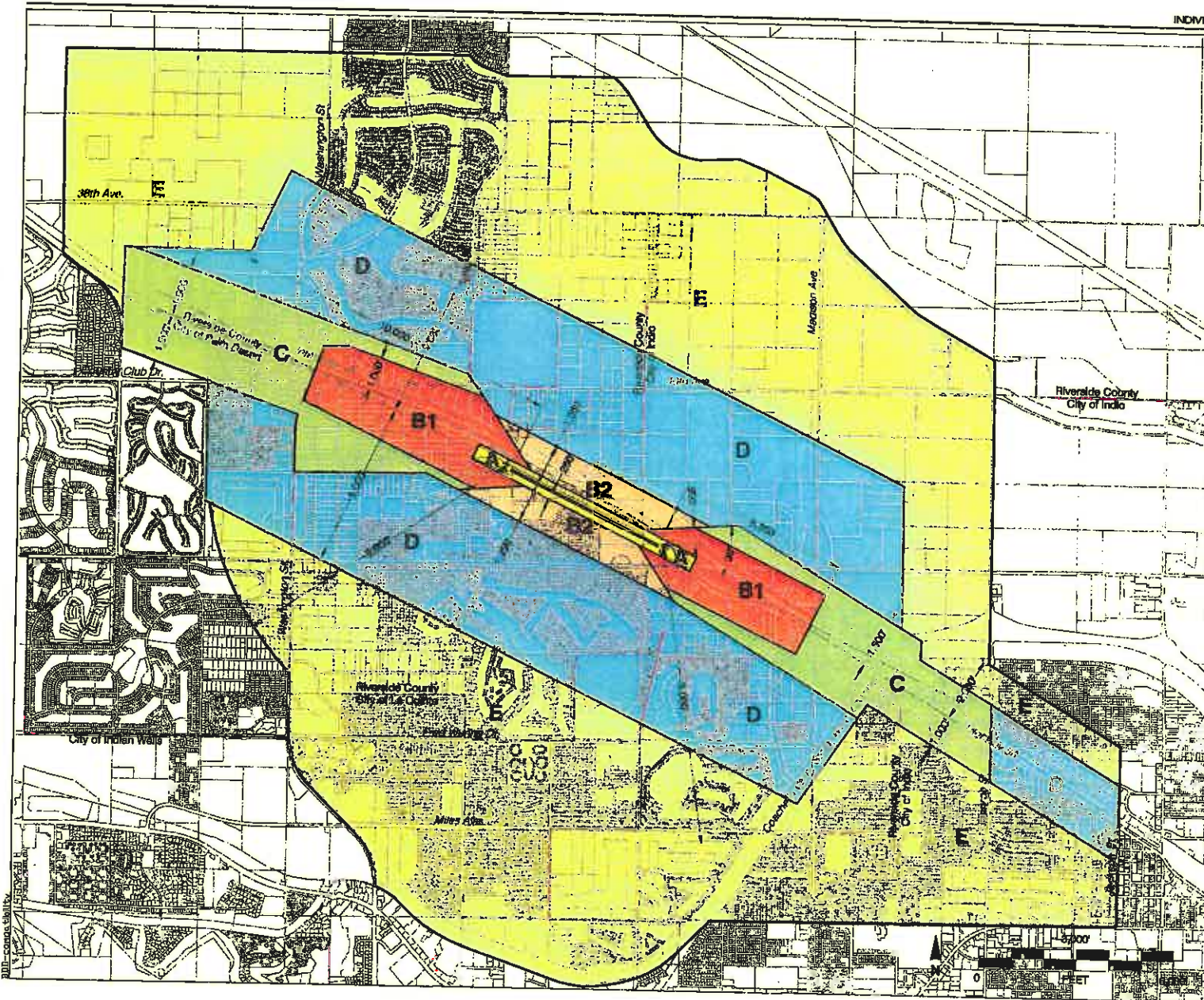
- **Infill Development**—If an ALUCP contains infill policies and a jurisdiction wishes to take advantage of them, the lands that meet the qualifications must be shown on a map.

Pages 3-29, 4-35

- **Height Limitations and Other Hazards to Flight**—To protect the airport airspace, limitations must be set on the height of structures and other objects near airports. These limitations are to be based upon FAR Part 77. Restrictions also must be established on other land use characteristics which can cause hazards to flight (specifically, visual or electronic interference with navigation and uses which attract birds). Note that many jurisdictions have already adopted an airport-related hazard and height limit zoning ordinance which, if up to date, will satisfy this consistency requirement.

TABLE 5A: GENERAL PLAN CONSISTENCY CHECKLIST

For additional guidance see:	COMPATIBILITY CRITERIA
Pages 3-9, 4-14	<ul style="list-style-type: none"> • Buyer Awareness Measures—Besides disclosure rules already required by state law, as a condition for approval of development within certain compatibility zones, some ALUCPs require either dedication of an aviation easement to the airport proprietor or placement on deeds of a notice regarding airport impacts. If so, local agency policies must contain similar requirements.
Page 4-42	<ul style="list-style-type: none"> • Nonconforming Uses and Reconstruction—Local agency policies regarding nonconforming uses and reconstruction must be equivalent to or more restrictive than those in the ALUCP, if any.
REVIEW PROCEDURES	
<p>In addition to incorporation of ALUC compatibility criteria, local agency implementing documents must specify the manner in which development proposals will be reviewed for consistency with the compatibility criteria.</p>	
Page 6-1	<ul style="list-style-type: none"> • Actions Always Required to be Submitted for ALUC Review—PUC Section 21676 identifies the types of actions that must be submitted for airport land use commission review. Local policies should either list these actions or, at a minimum, note the local agency's intent to comply with the state statute.
Page 6-5	<ul style="list-style-type: none"> • Other Land Use Actions Potentially Subject to ALUC Review—In addition to the above actions, ALUCPs may identify certain major land use actions for which referral to the ALUC is dependent upon agreement between the local agency and ALUC. If the local agency fully complies with all of the items in this general plan consistency check list or has taken the necessary steps to overrule the ALUC, then referral of the additional actions is voluntary. On the other hand, a local agency may elect not to incorporate all of the necessary compatibility criteria and review procedures into its own policies. In this case, referral of major land use actions to the ALUC is mandatory. Local policies should indicate the local agency's intentions in this regard.
Pages 5-10, 6-13	<ul style="list-style-type: none"> • Process for Compatibility Reviews by Local Agencies—If a local agency chooses to submit only the mandatory actions for ALUC review, then it must establish a policy indicating the procedures which will be used to assure that airport compatibility criteria are addressed during review of other projects. Possibilities include: a standard review procedure checklist which includes reference to compatibility criteria; use of a geographic information system to identify all parcels within the airport influence area; etc.
Page 6-9	<ul style="list-style-type: none"> • Variance Procedures—Local procedures for granting of variances to the zoning ordinance must make certain that any such variances do not result in a conflict with the compatibility criteria. Any variance that involves issues of noise, safety, airspace protection, or overflight compatibility as addressed in the ALUCP must be referred to the ALUC for review.
Page 5-10	<ul style="list-style-type: none"> • Enforcement—Policies must be established to assure compliance with compatibility criteria during the lifetime of the development. Enforcement procedures are especially necessary with regard to limitations on usage intensities and the heights of trees. An airport combining district zoning ordinance is one means of implementing enforcement requirements.



Legend

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E

Boundary Lines

- Airport Property Line
- City Limits

Note

Southwestern edge of Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

See Chapter 2, Table 2A for compatibility criteria associated with this map.

Riverside County
Airport Land Use Commission
Riverside County
Airport Land Use Compatibility Plan
Policy Document
(Adopted December 2004)

Map BD-1

Compatibility Map
Bermuda Dunes Airport

Figure 3-5: General Plan Place Types

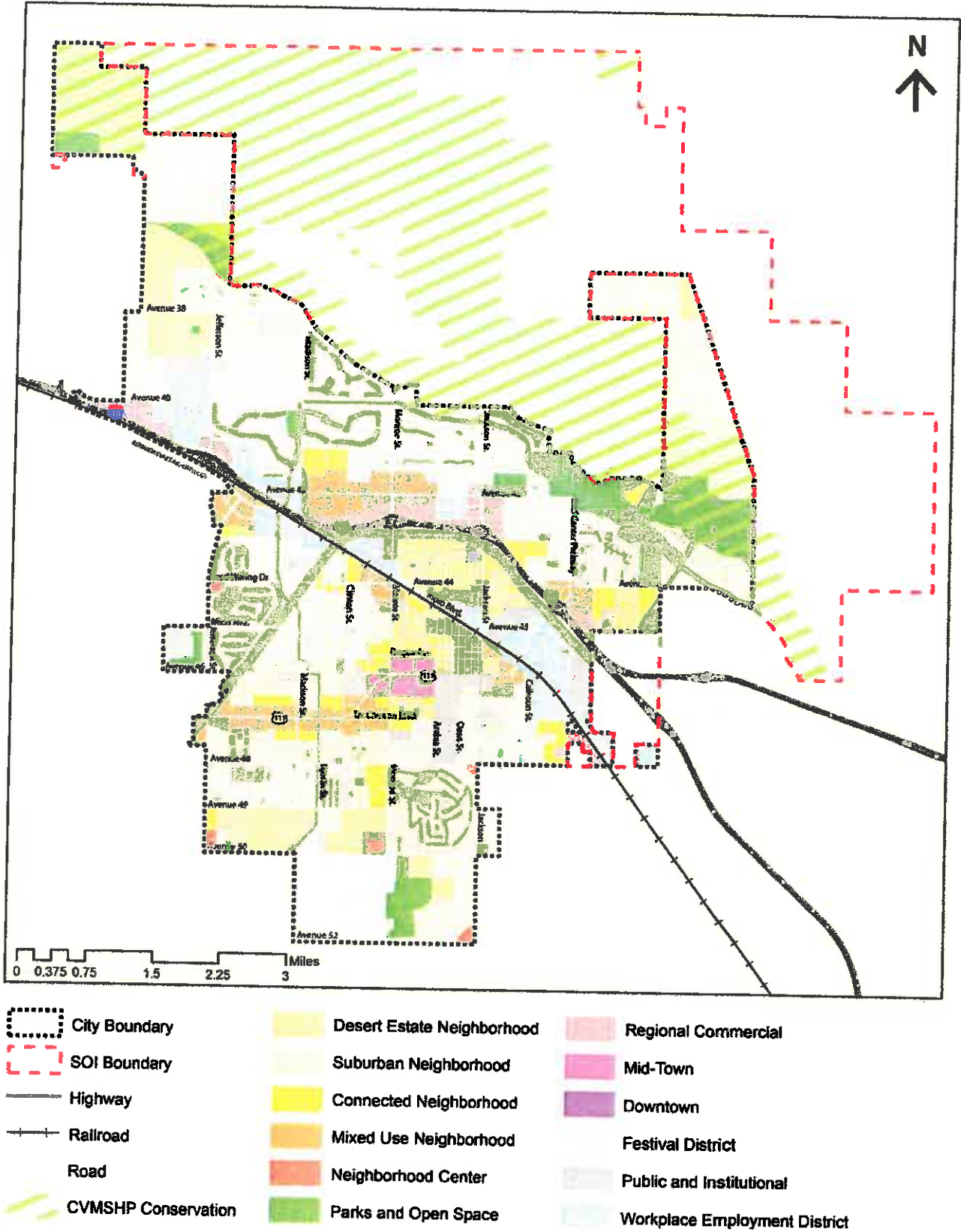
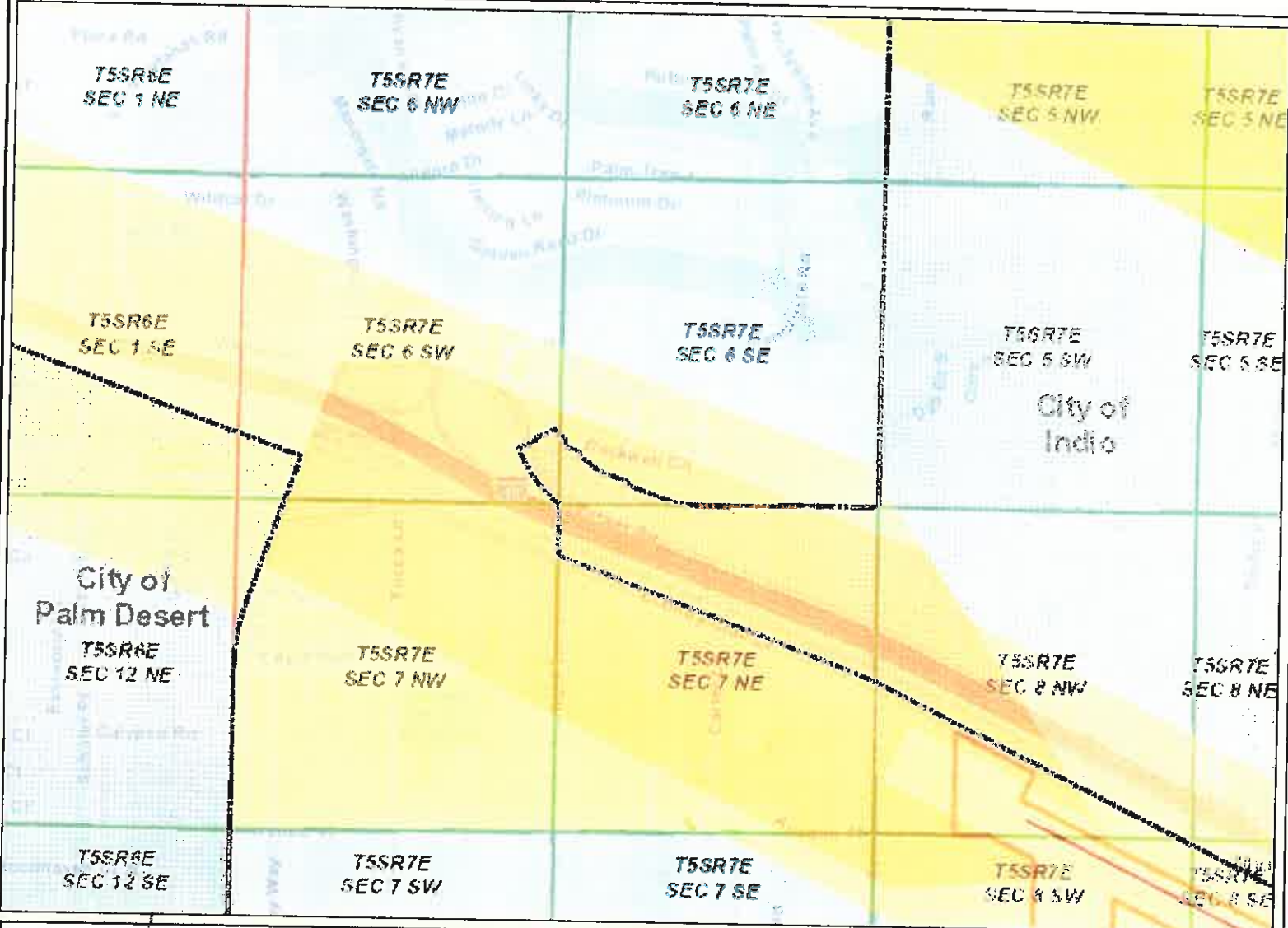


Table 3-2: General Plan Place Types

Major Classes	Place Type and Intent	Max. Gross Density/FAR*	Zoning Correlation
NEIGHBORHOODS	Desert Estates (DE). Conserves natural features while providing the lowest intensity residential neighborhood development.	DU/AC: Up to 1	EE, CEIR, CET, CE
	Suburban Neighborhoods (SN). Provides low-intensity neighborhood development for single-family, detached homes.	Up to 8 DU/AC	EE, CEIR, CET, CE, RL, RM
	Connected Neighborhood (CN). Provides a broad range of housing choices within a walkable neighborhood setting within a short distance of goods and services. Housing types include single-family detached and attached dwellings of various sizes, courtyard apartments and condos, and medium-sized multifamily buildings designed for neighborhood compatibility.	DU/AC: Up to 20 FAR: up to 0.25	RLCI, RM, CE, RM, RH, P
	Mixed Use Neighborhood (MUN). Provides moderate- to higher-intensity neighborhood development that features a variety of multifamily housing choices and commercial uses along major streets.	DU/AC: Up to 40 FAR: up to 0.5	RH, NC, CC, P
CENTERS	Downtown (DT). Supports lively, thriving Downtown area by accommodating multi-story, mixed-use buildings at higher intensities. Provides for vertical and horizontal mixed-use development.	DU/AC: Up to 60 FAR: Up to 2.0	DC, P, RH
	Midtown (MT). Provides of an active, mixed-use center in Midtown by allowing multi-story, mixed-use buildings at higher intensities, community gathering spaces, and land uses to support residents and visitors. Provides for vertical and horizontal mixed-use development.	DU/AC: Up to 40 FAR: Up to 1.0	NC, CC, RH, P
	Neighborhood Center (NC). Provides for areas with a variety of neighborhood-serving retail and commercial uses, housing of various types, other neighborhood-serving amenities, and community-gathering spaces for residents.	DU/AC: 10 to 20 FAR: Up to 0.5	NC, CC, RH, P
DISTRICTS	Workplace and Employment District (WEP). Provides an area for a wide variety of employment-generating activity, including office, industrial and light manufacturing, research and development, and supportive commercial.	Up 1.0 FAR	CO, BP, IP, M, NC
	Festival District (FD). Reserves area for outdoor festivals, events, sporting activities and related activities.	Up to 0.5 FAR	NC, MMFOZ
	Regional Commercial (RC). Provides large-format retail development along with associated restaurant and commercial service activity.	FAR: 0.35 to 1.0	RC, CC
PUBLIC AND INSTITUTIONAL	Public and Institutional (PI). Reserves areas for public, educational, and institutional uses.	Up to 0.5 FAR	P
	Parks and Open Space (OS). Provides public and private space for recreational uses, including open space, and golf course greens.	Up to 0.01 FAR	P, OS
	Resource Management and Open Space Overlay (R-OS). Provides an overlay for areas consistent with the Coachella Valley Multi-Species Habitat Management Plan, passive recreation, trails/paths, restoration, ecotourism, and environmental educational activities.	Consistent with CVMSHMP	P, OS, RR

*For larger development projects, portions of the project area may exceed the density as long as the total project density does not exceed the allowable density or FAR for the placetype.

Map My County Map



- ### Legend
- Runways
 - Airports
 - Airport Influence Areas
 - Airport Compatibility Zones**
 - OTHER COMPATIBILITY ZONE
 - A
 - A-EXC1
 - B1
 - B1-APZ I
 - B1-APZ I-EXC1
 - B1-APZ II
 - B1-APZ II-EXC1
 - B1-EXC1
 - B2
 - B2-EXC1
 - C
 - C1
 - C1-EXC1
 - C1-EXC3
 - C1-EXC4
 - C1-HIGHT
 - C2
 - C2-EXC1
 - C2-EXC2
 - C2-EXC3
 - C2-EXC5
 - C2-EXC6



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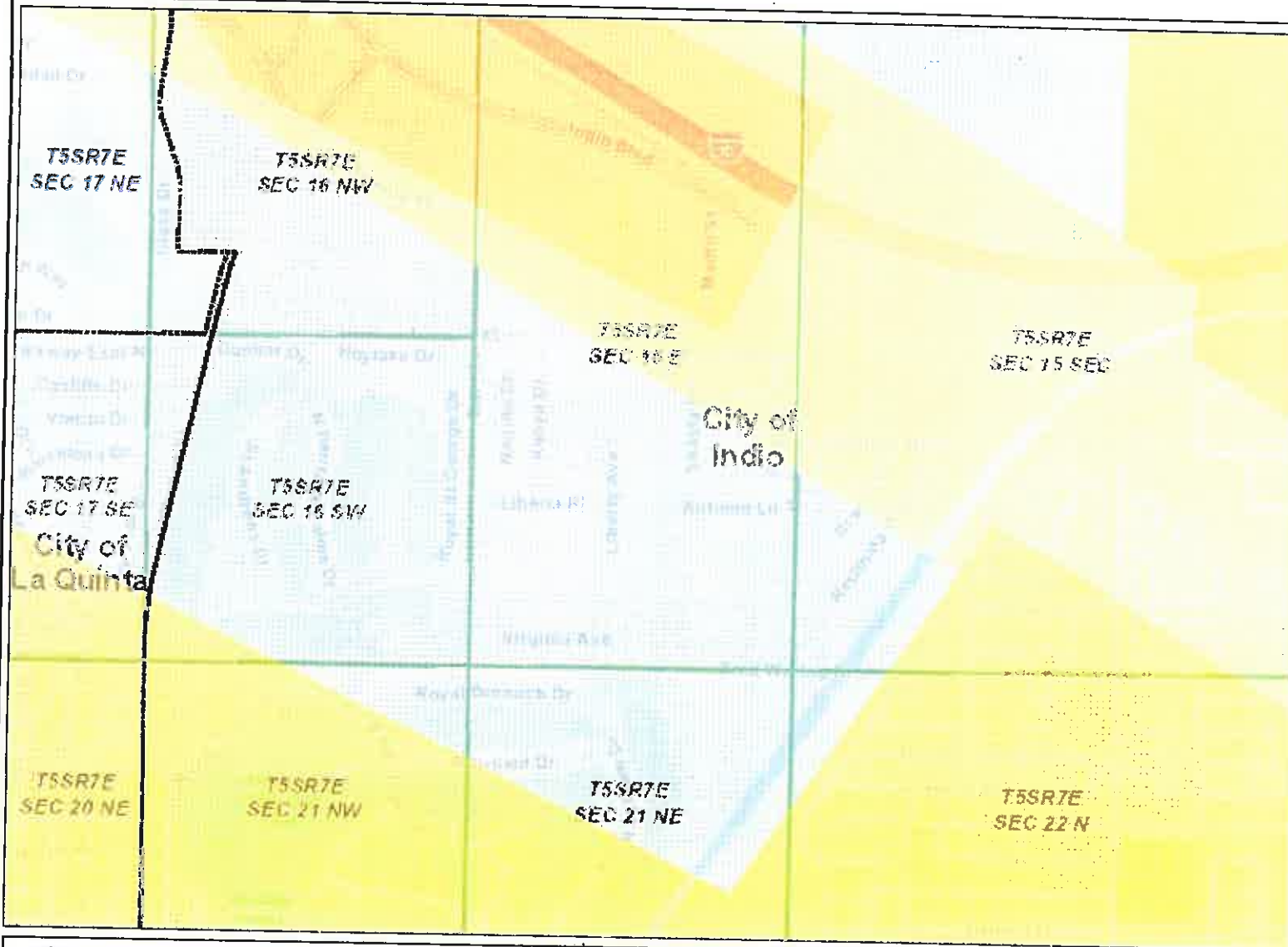


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Notes

Map My County Map



Los Angeles



San Diego

Tijuana

Mexicali

Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
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- C2-EXC5
- C2-EXC6



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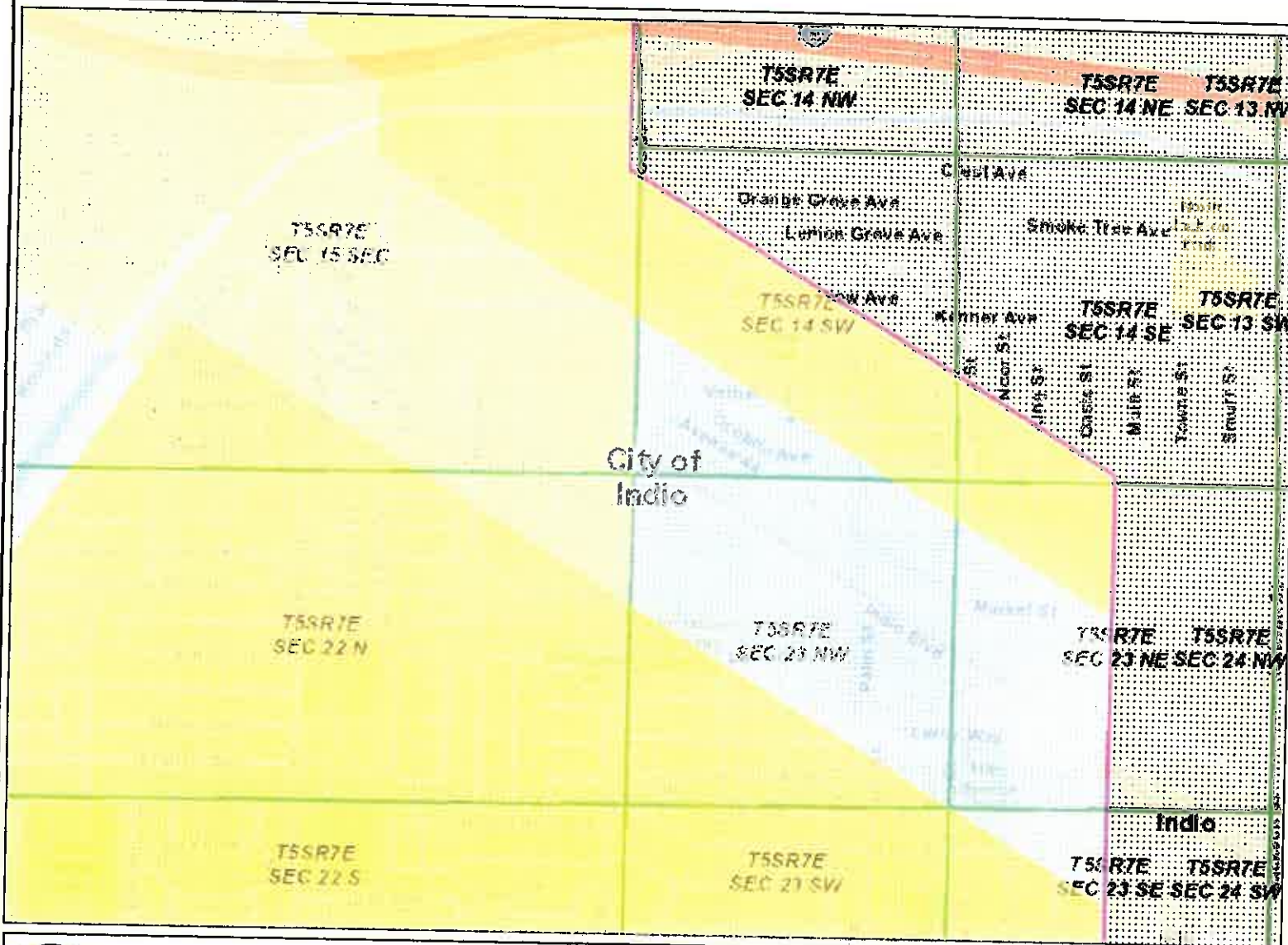
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Notes

Map My County Map



- ### Legend
- Runways
 - Airports
 - Airport Influence Areas
 - Airport Compatibility Zones**
 - OTHER COMPATIBILITY ZONE
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 - B1-EXC1
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 - C
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 - C2-EXC5
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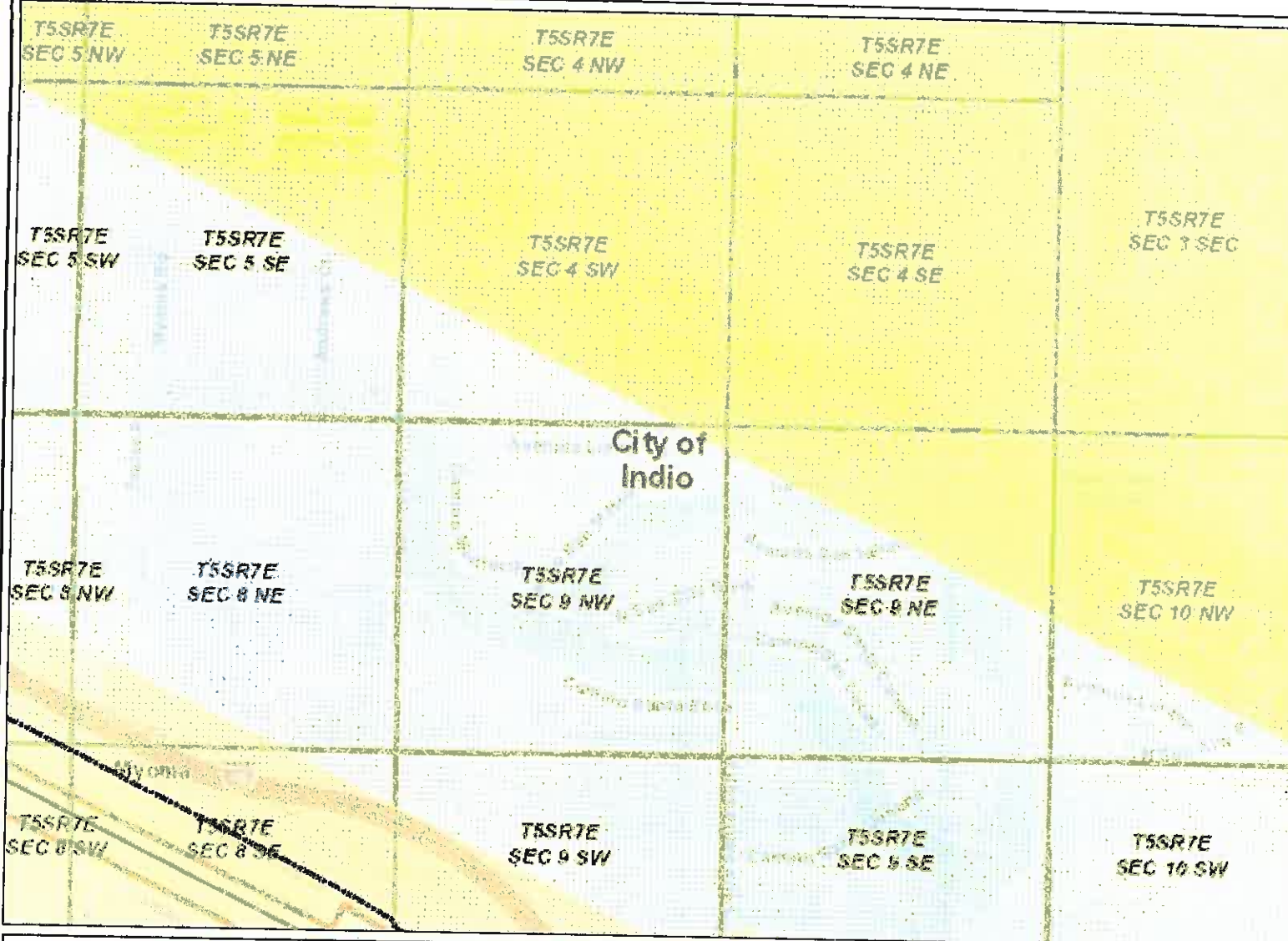


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 - Airports
 - Airport Influence Areas
 - Airport Compatibility Zones**
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 - C2-EXC5
 - C2-EXC6



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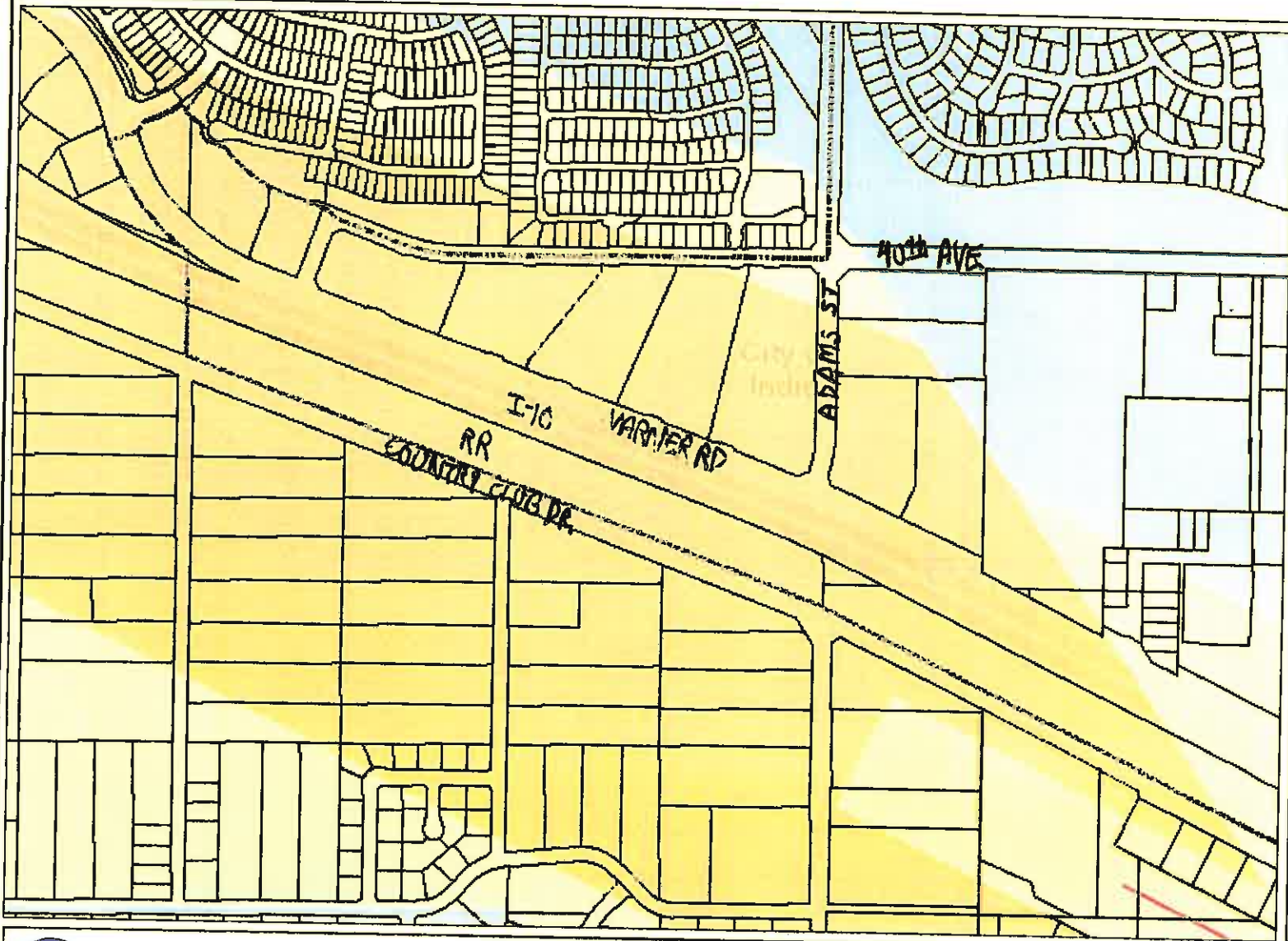


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- Legend**
- Parcels
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 - Airports
 - Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
 - A
 - A-EXC1
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 - B1-APZ I
 - B1-APZ I-EXC1
 - B1-APZ II
 - B1-APZ II-EXC1
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 - C1-EXC4
 - C1-HIGHT
 - C2
 - C2-EXC1
 - C2-EXC2
 - C2-EXC3
 - C2-EXC5



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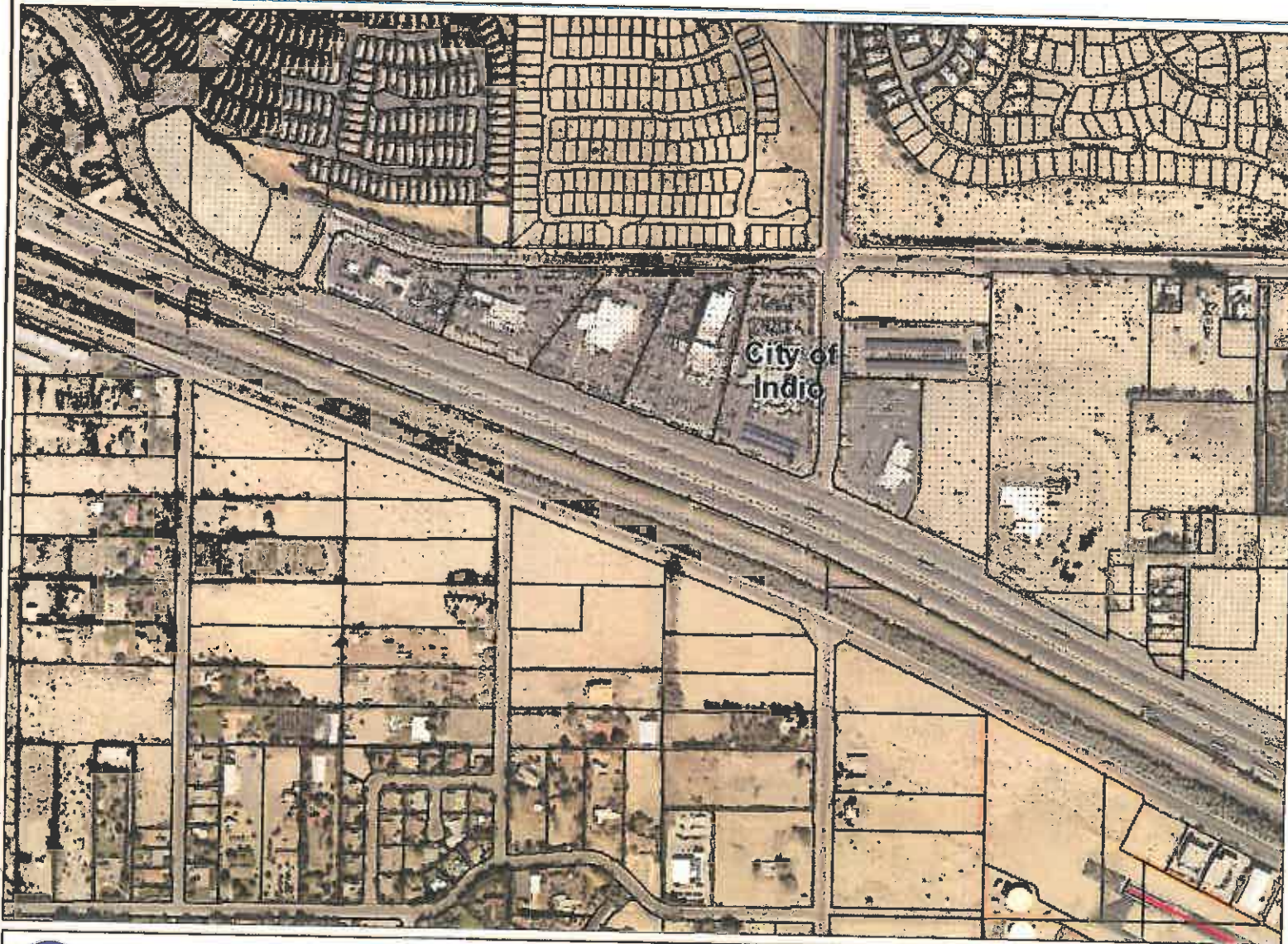


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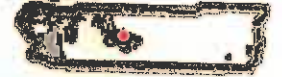
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Notes

Map My County Map



Los Angeles



San Diego

Tijuana

Mexico

Legend

-  Parcels
-  Runways
-  Airports
-  Airport Influence Areas
-  Blueline Streams
-  Townships
-  City Areas
-  World Street Map



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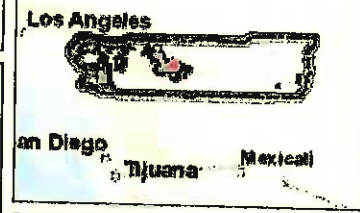
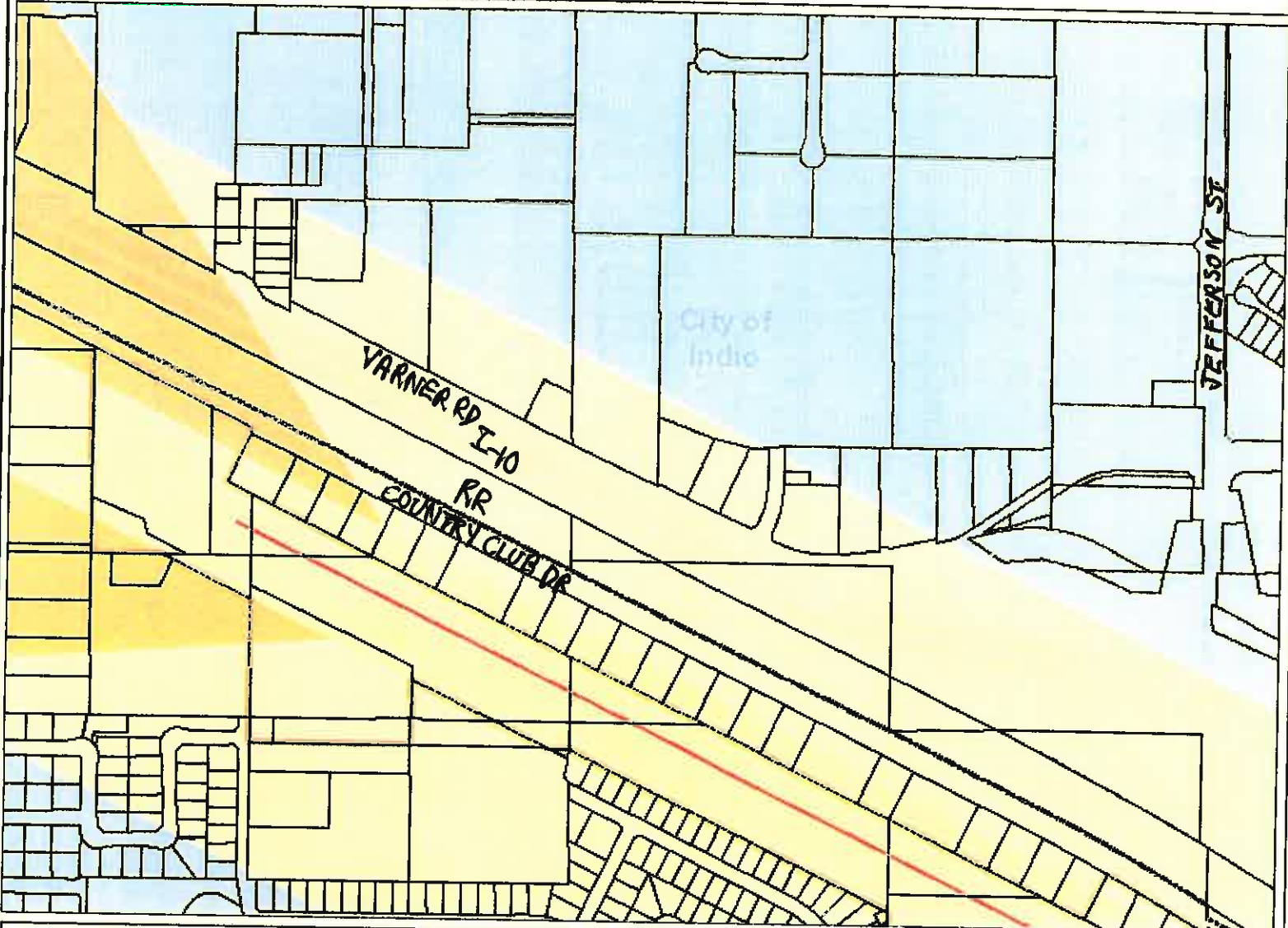
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Notes

Map My County Map





Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas

Airport Compatibility Zones

- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
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- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
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- C
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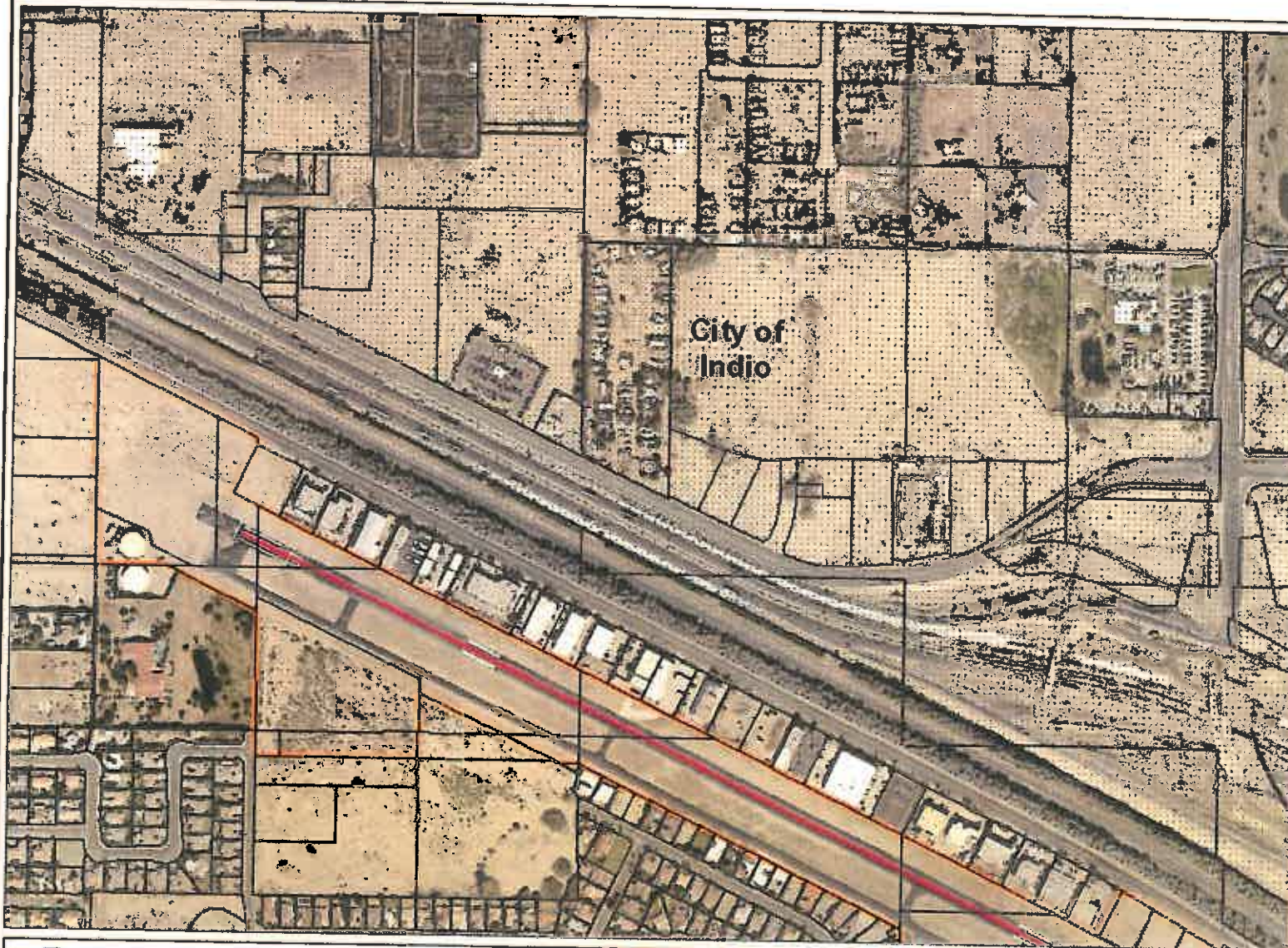



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Notes

Map My County Map



Legend

-  Parcels
-  Runways
-  Airports
-  Airport Influence Areas
-  Blueline Streams
-  Townships
-  City Areas
-  World Street Map



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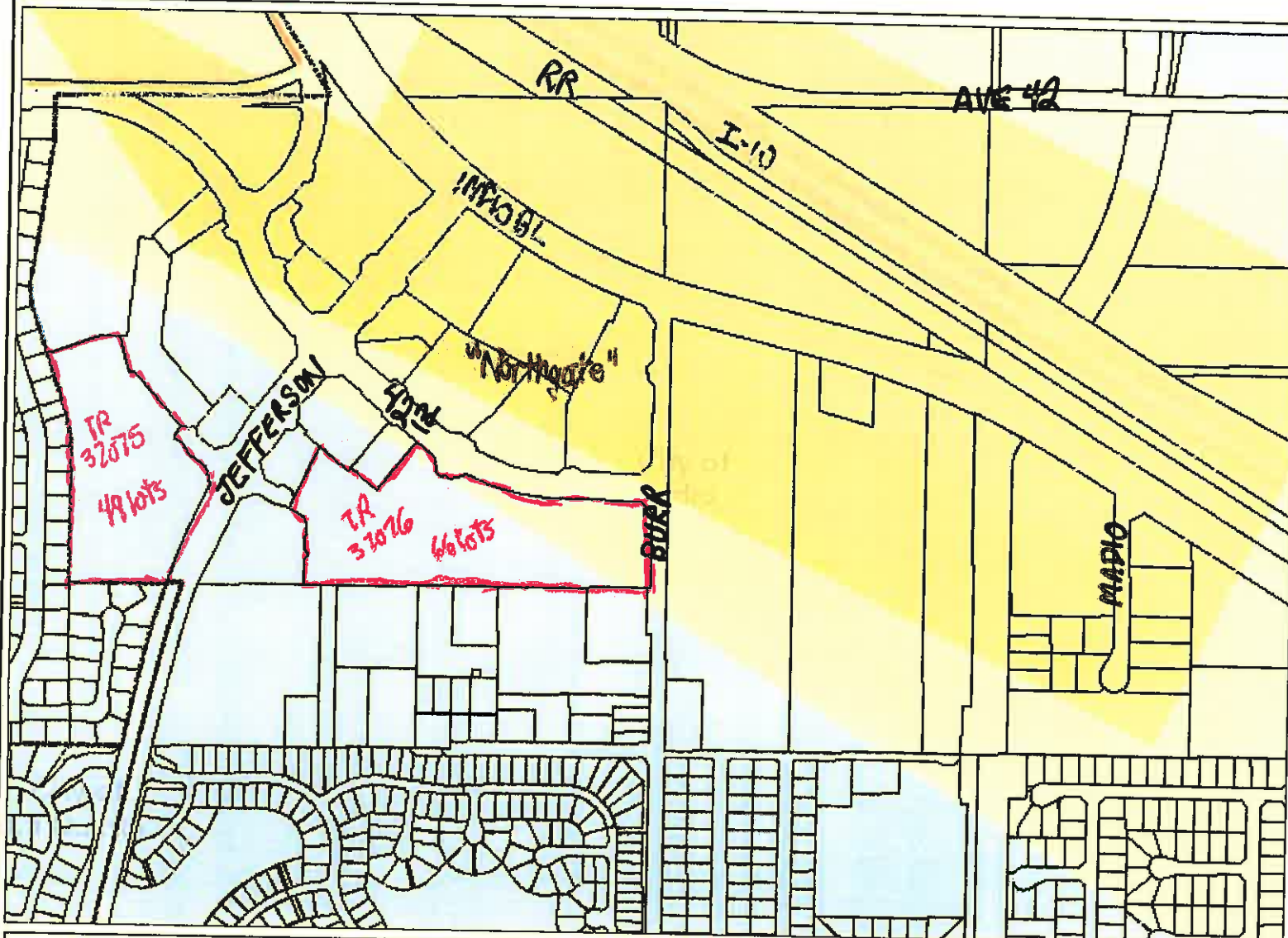
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Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
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- A-EXC1
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- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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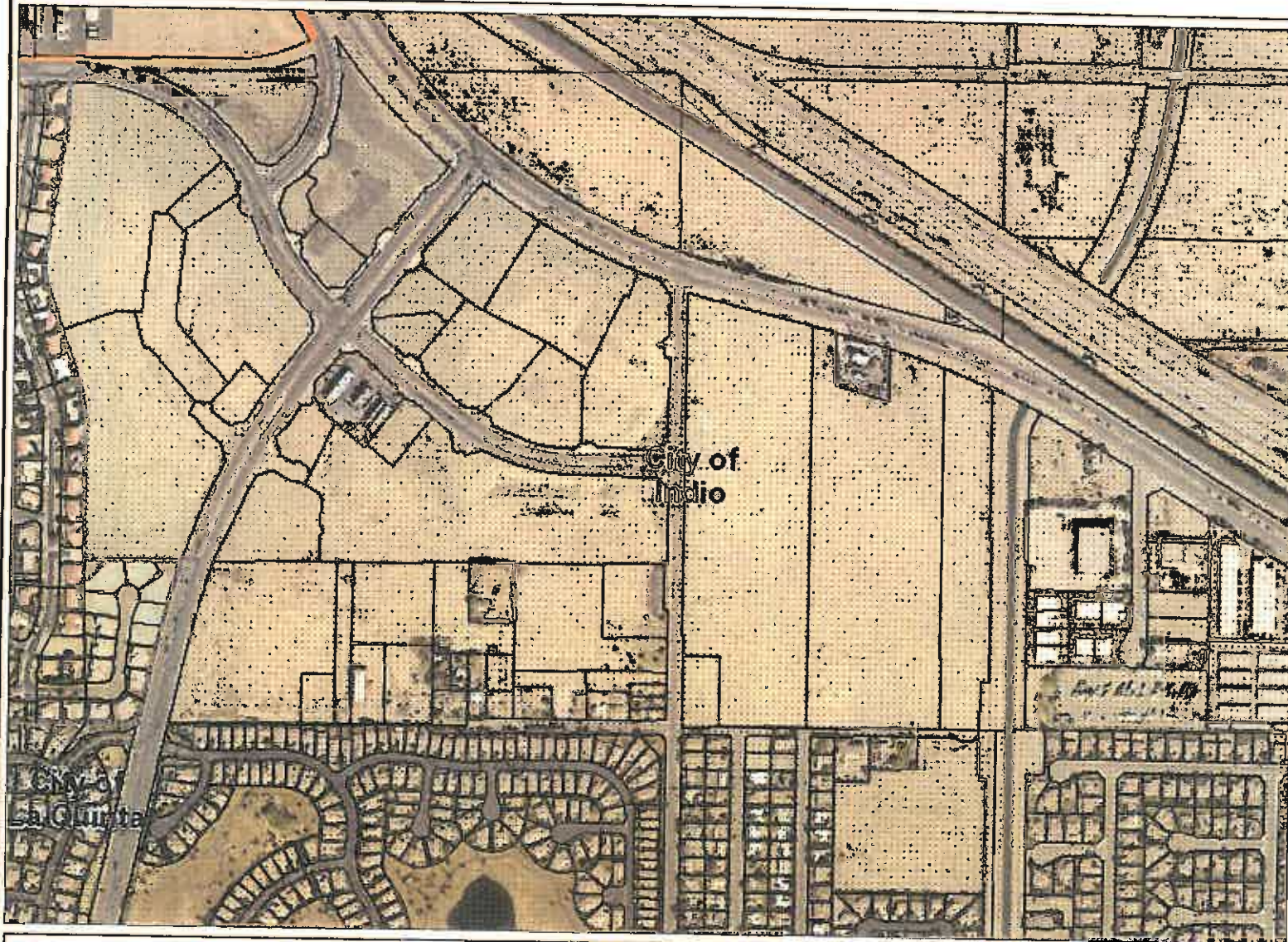


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Notes

Map My County Map



Legend

-  Parcels
-  Runways
-  Airports
-  Airport Influence Areas
-  Blueline Streams
-  Townships
-  City Areas
-  World Street Map



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0 752 1,505 Feet

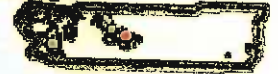
REPORT PRINTED ON... 8/15/2018 12:51:08 PM

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Notes

Map My County Map

Los Angeles

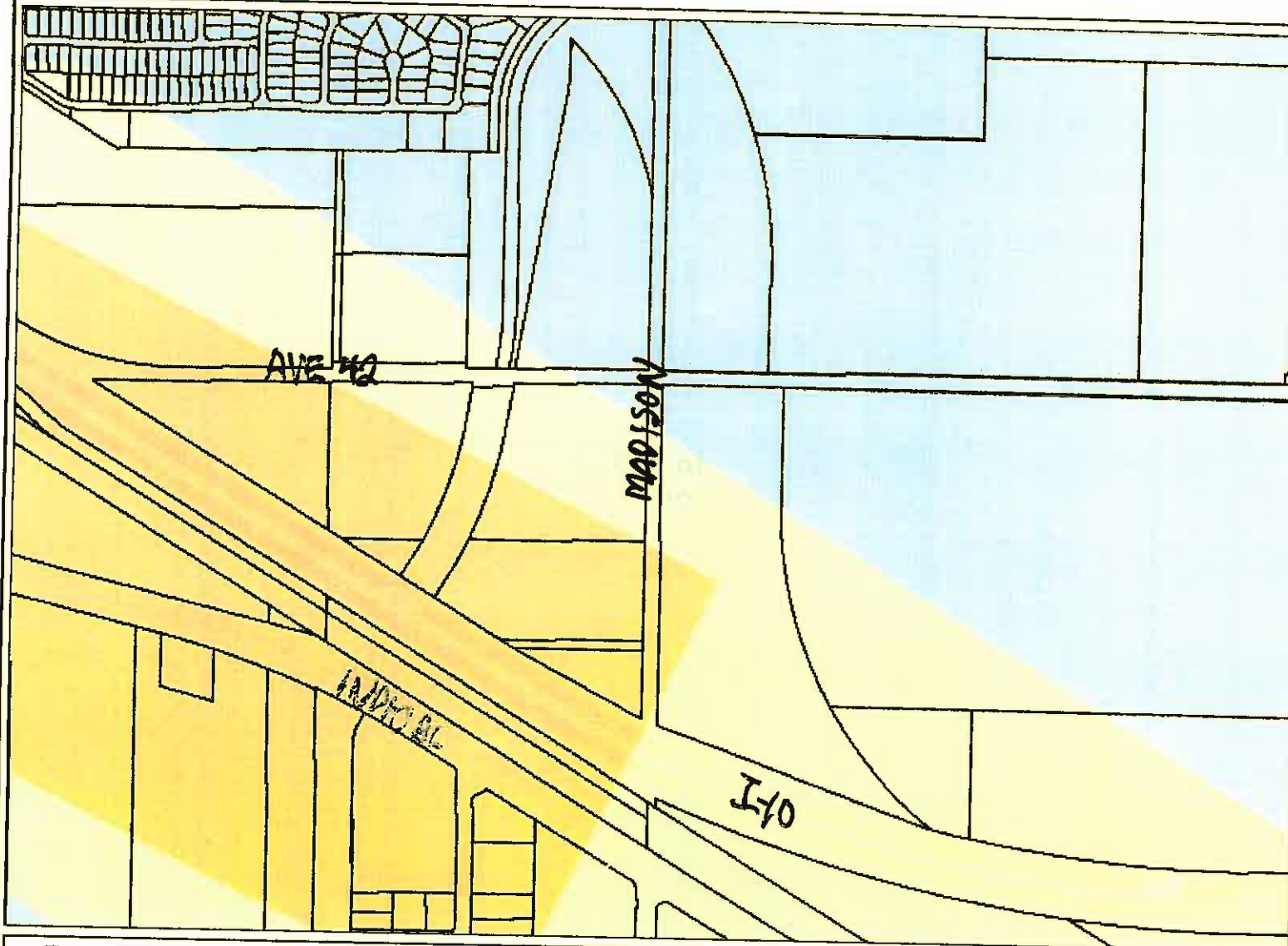


San Diego

Tijuana Mexico

Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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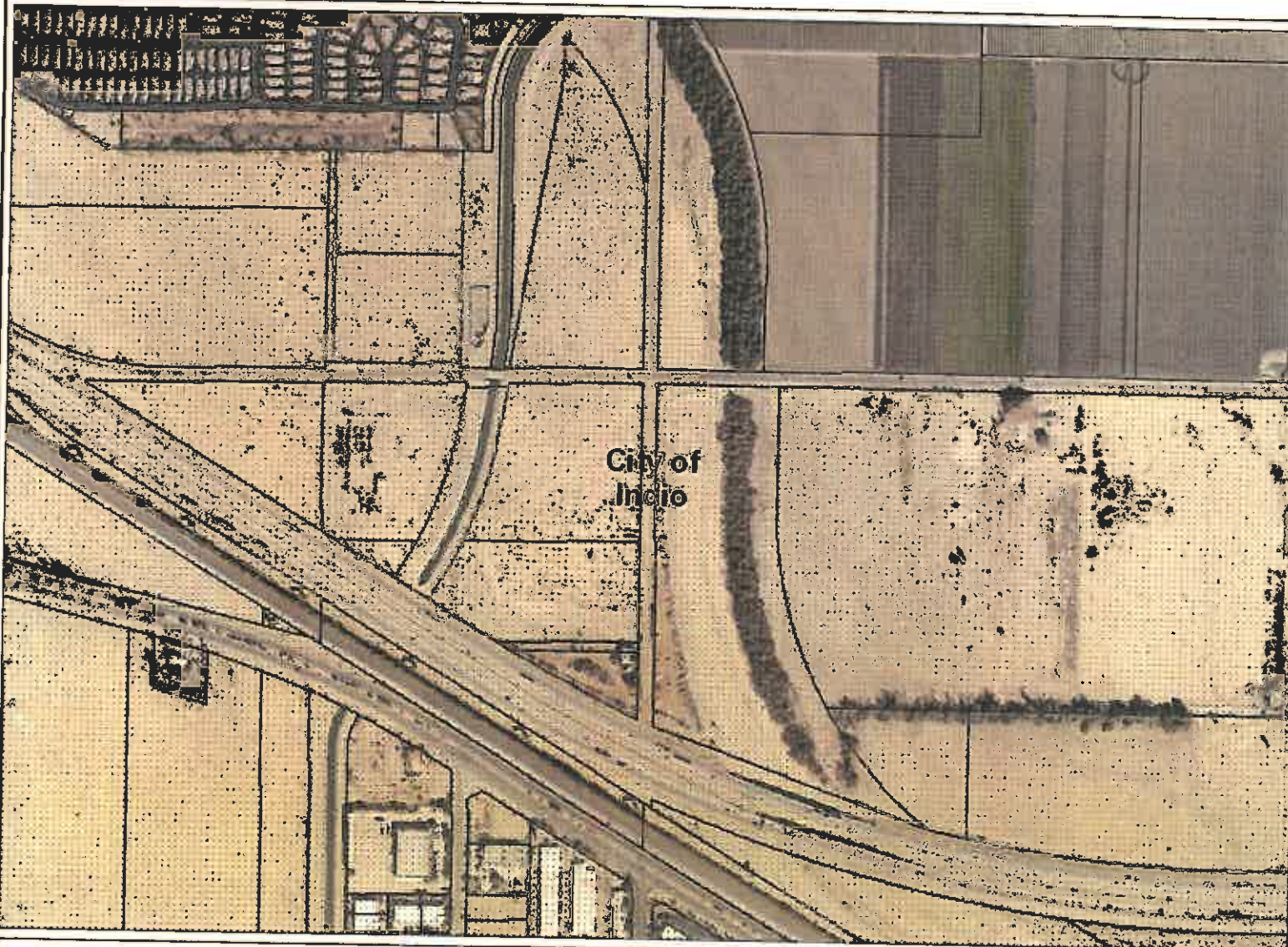
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Notes

Map My County Map



- Legend**
-  Parcels
 -  Runways
 -  Airports
 -  Airport Influence Areas
 -  Blueline Streams
 -  Townships
 -  City Areas
 -  World Street Map



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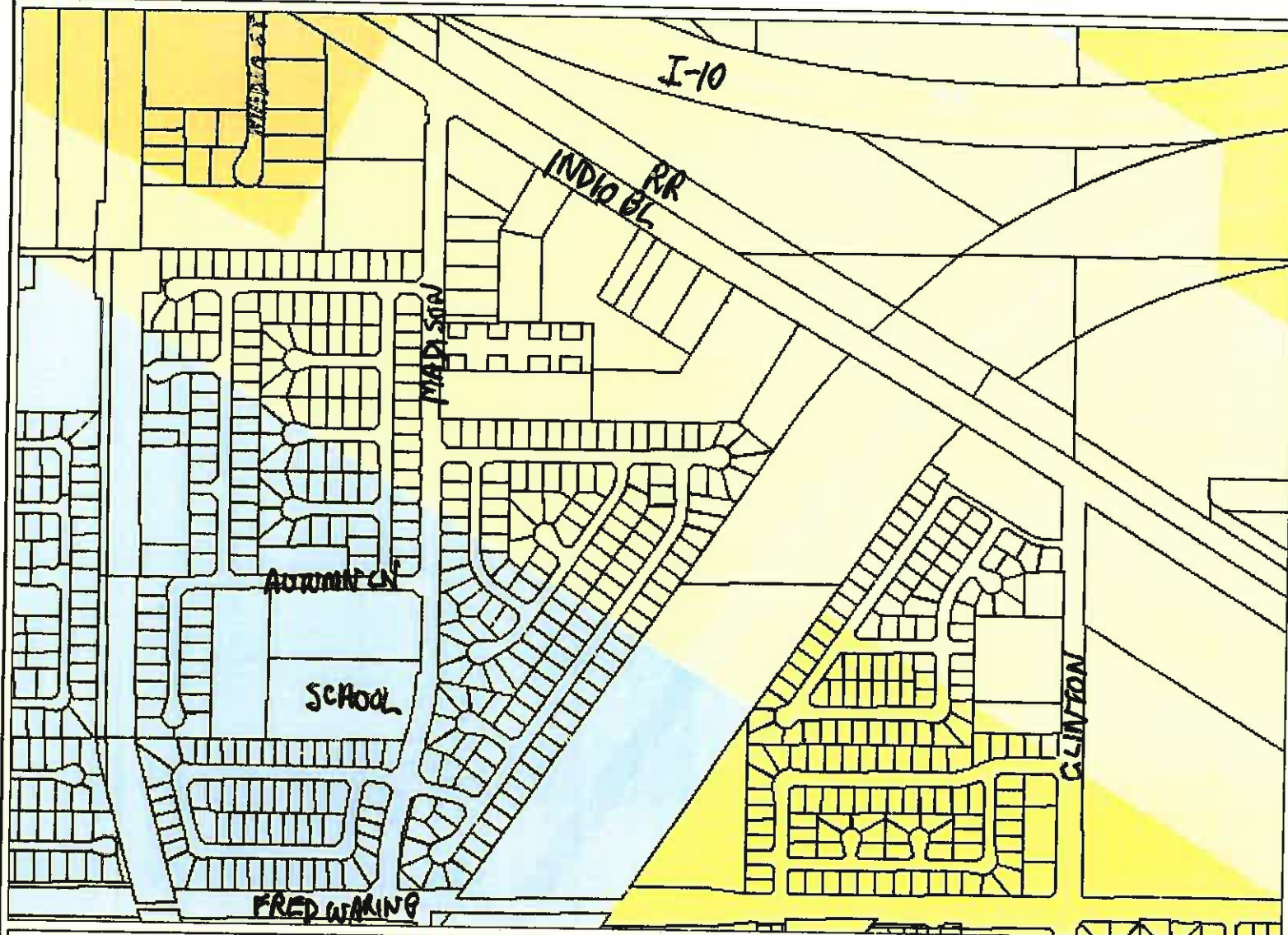


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Notes

Map My County Map



- Legend**
- Parcels
 - Runways
 - Airports
 - Airport Influence Areas
 - Airport Compatibility Zones**
 - OTHER COMPATIBILITY ZONE
 - A
 - A-EXC1
 - B1
 - B1-APZ I
 - B1-APZ I-EXC1
 - B1-APZ II
 - B1-APZ II-EXC1
 - B1-EXC1
 - B2
 - B2-EXC1
 - C
 - C1
 - C1-EXC1
 - C1-EXC3
 - C1-EXC4
 - C1-HIGHT
 - C2
 - C2-EXC1
 - C2-EXC2
 - C2-EXC3
 - C2-EXC5



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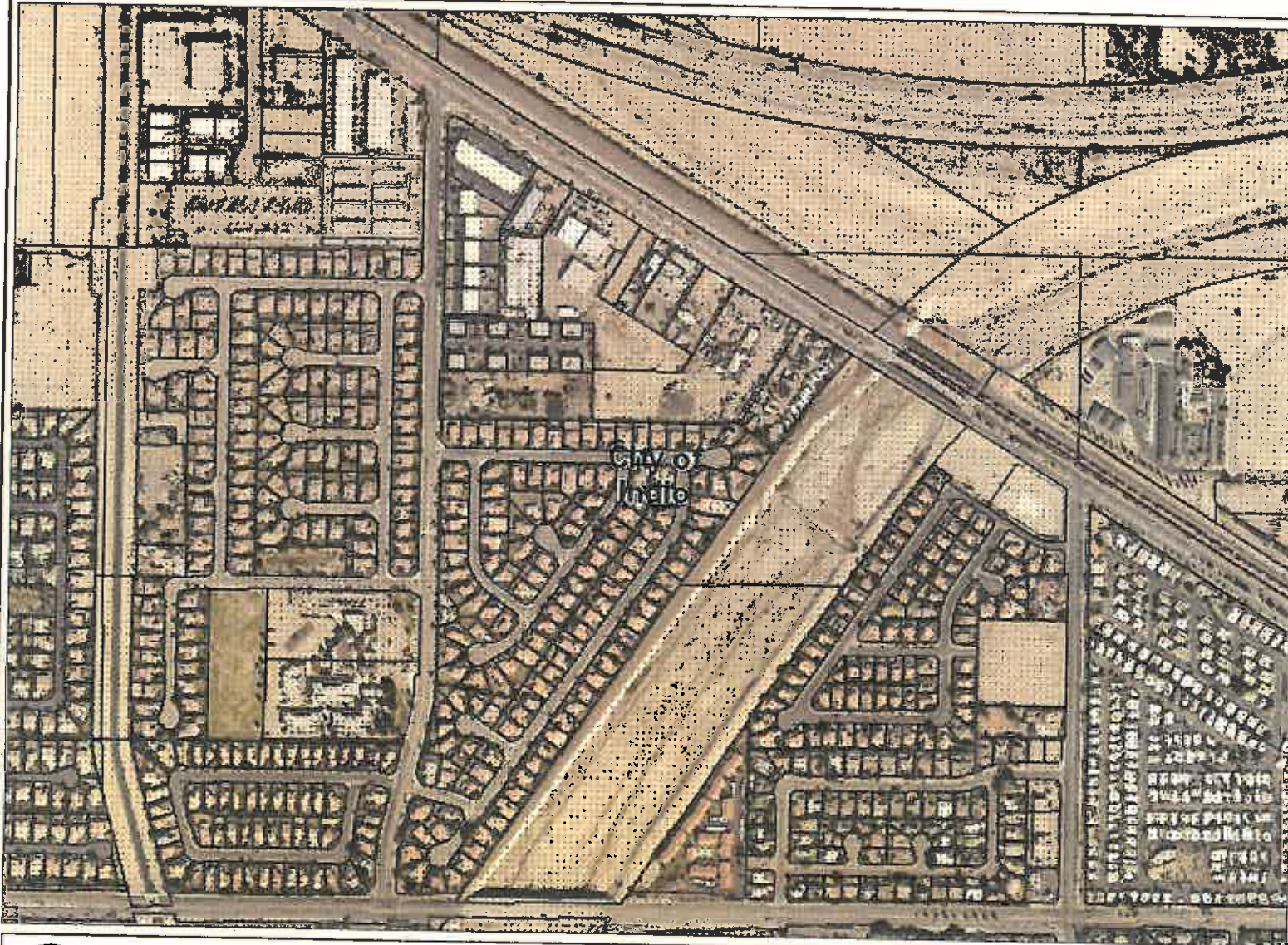


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Notes

Map My County Map



- Legend**
- Parcels
 - Runways
 - Airports
 - Airport Influence Areas
 - Blueline Streams
 - Townships
 - City Areas
 - World Street Map



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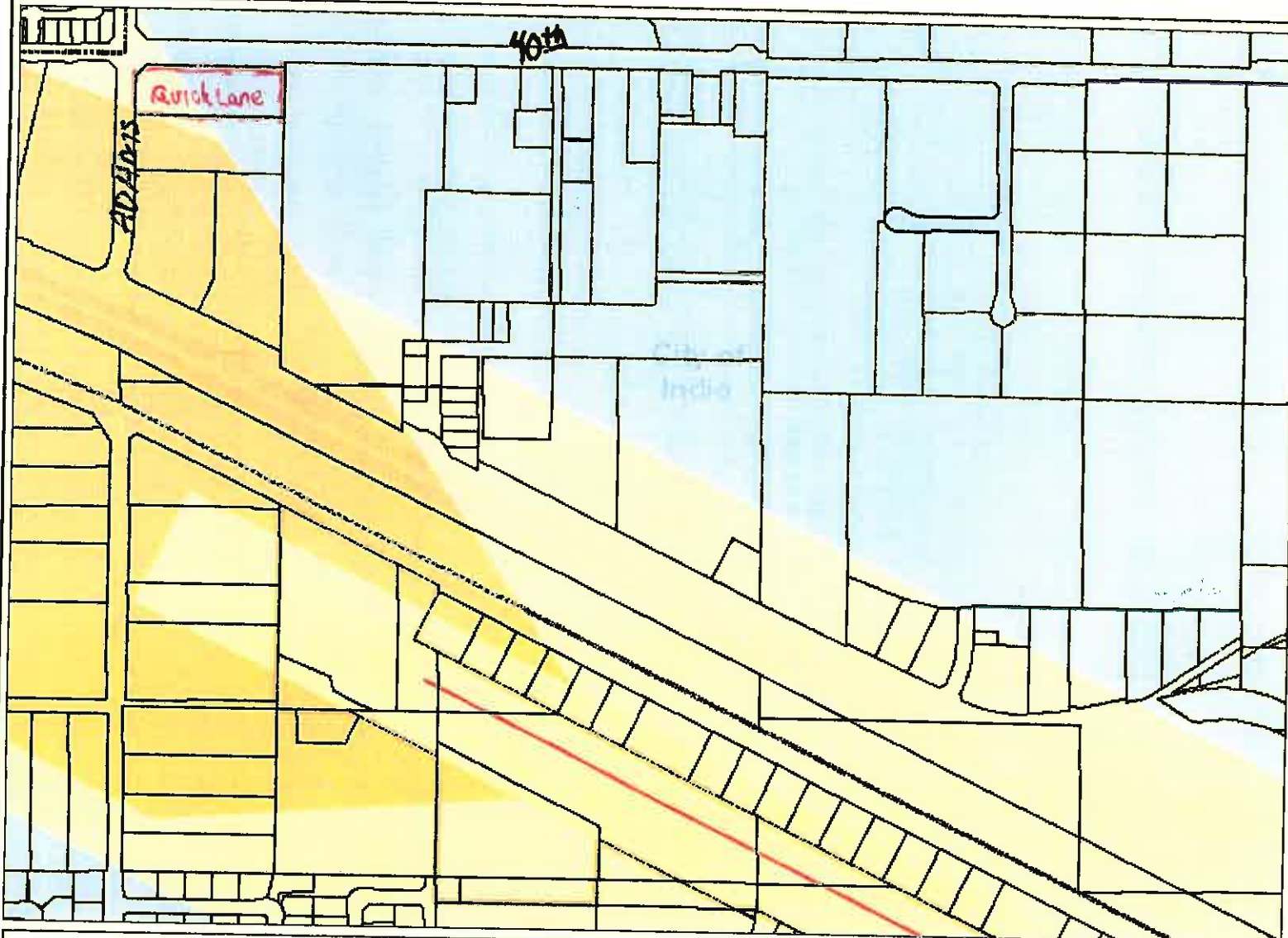
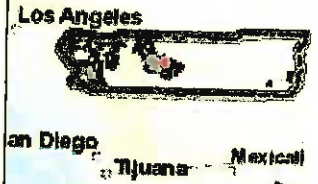
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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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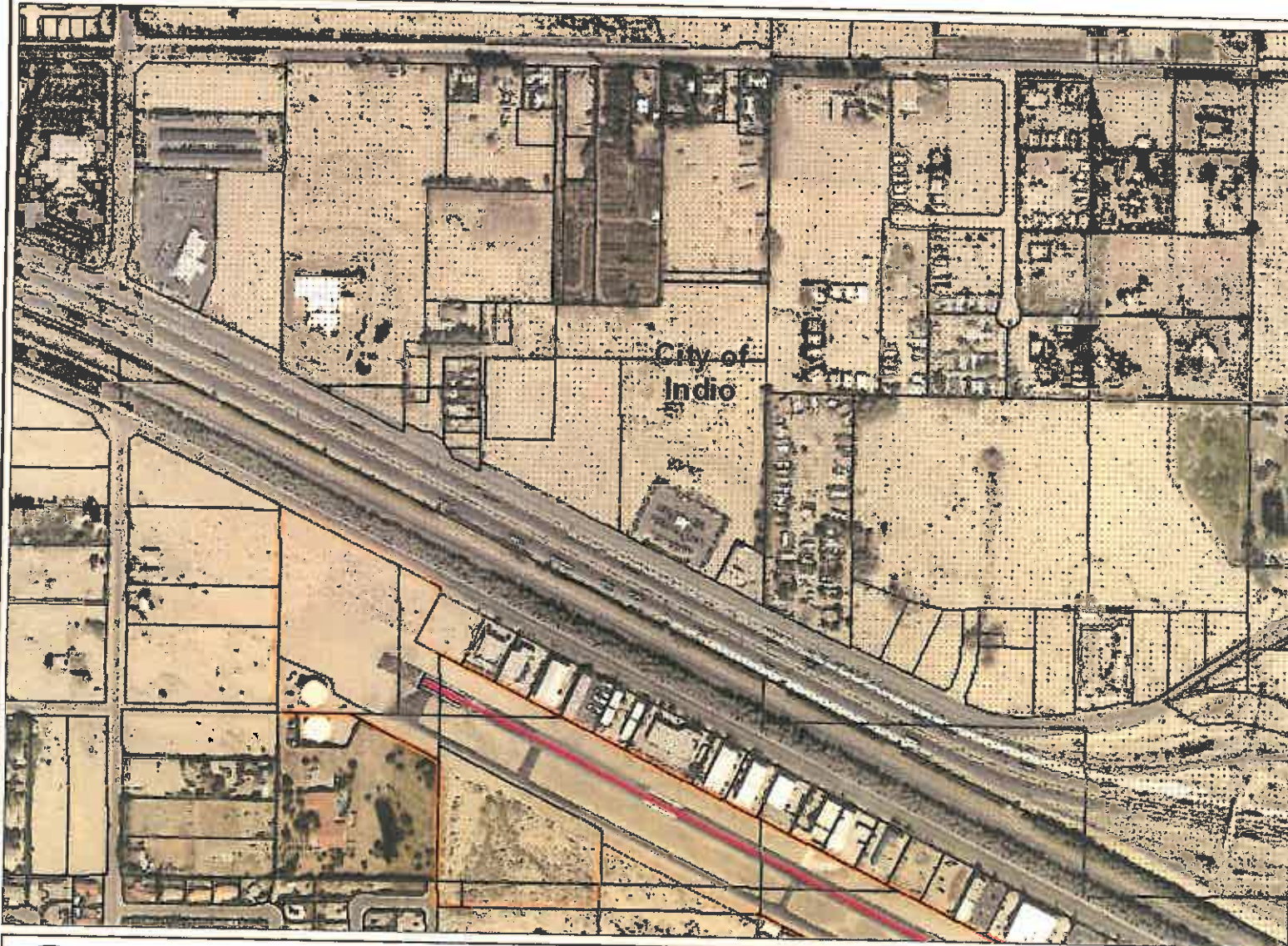


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Notes

Map My County Map



- Legend**
- Parcels
 - Runways
 - Airports
 - Airport Influence Areas
 - Blueline Streams
 - Townships
 - City Areas
 - World Street Map



0 752 1,505 Feet

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Notes

Map My County Map

Los Angeles

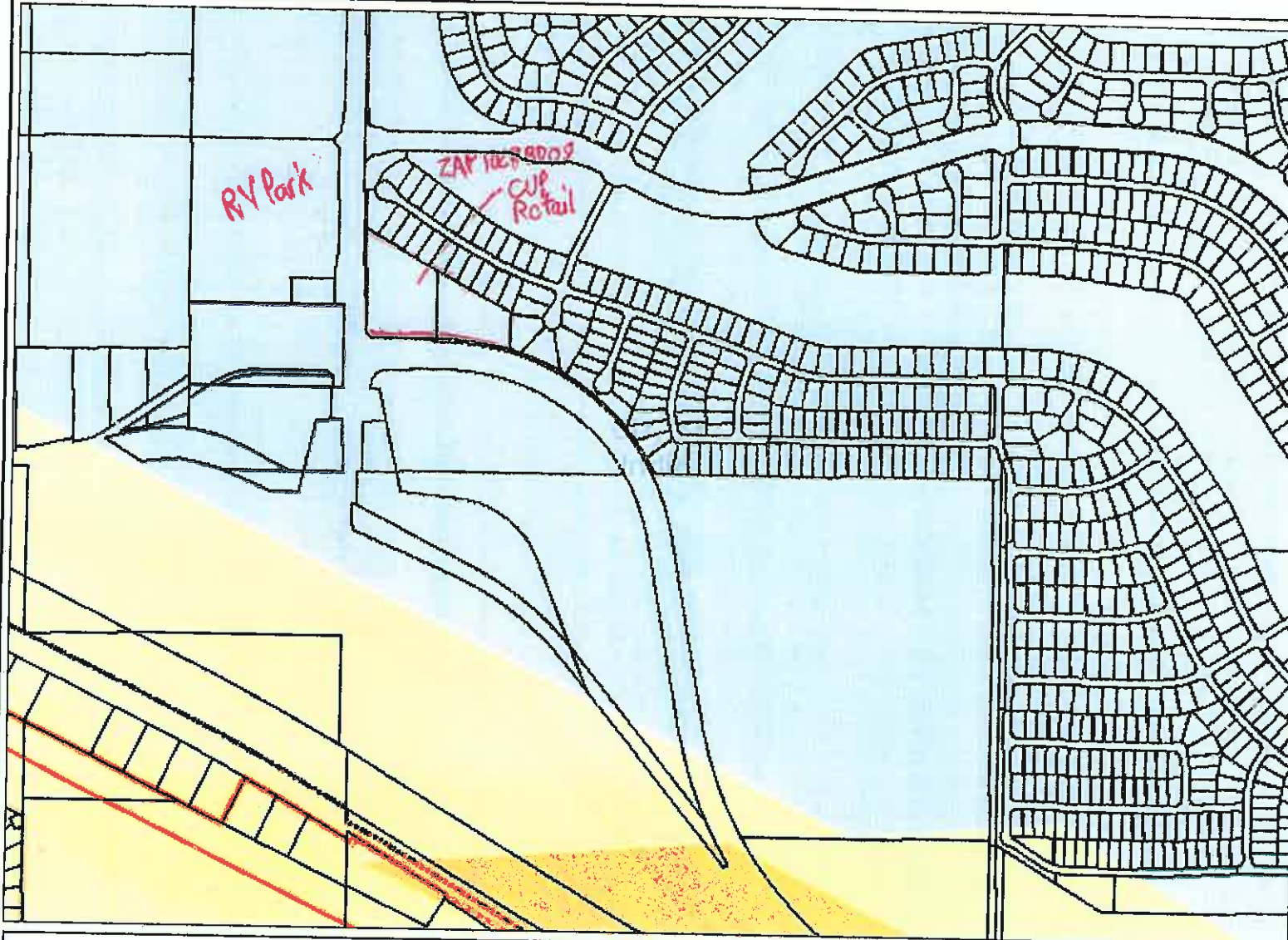


San Diego

Tijuana - Mexico

Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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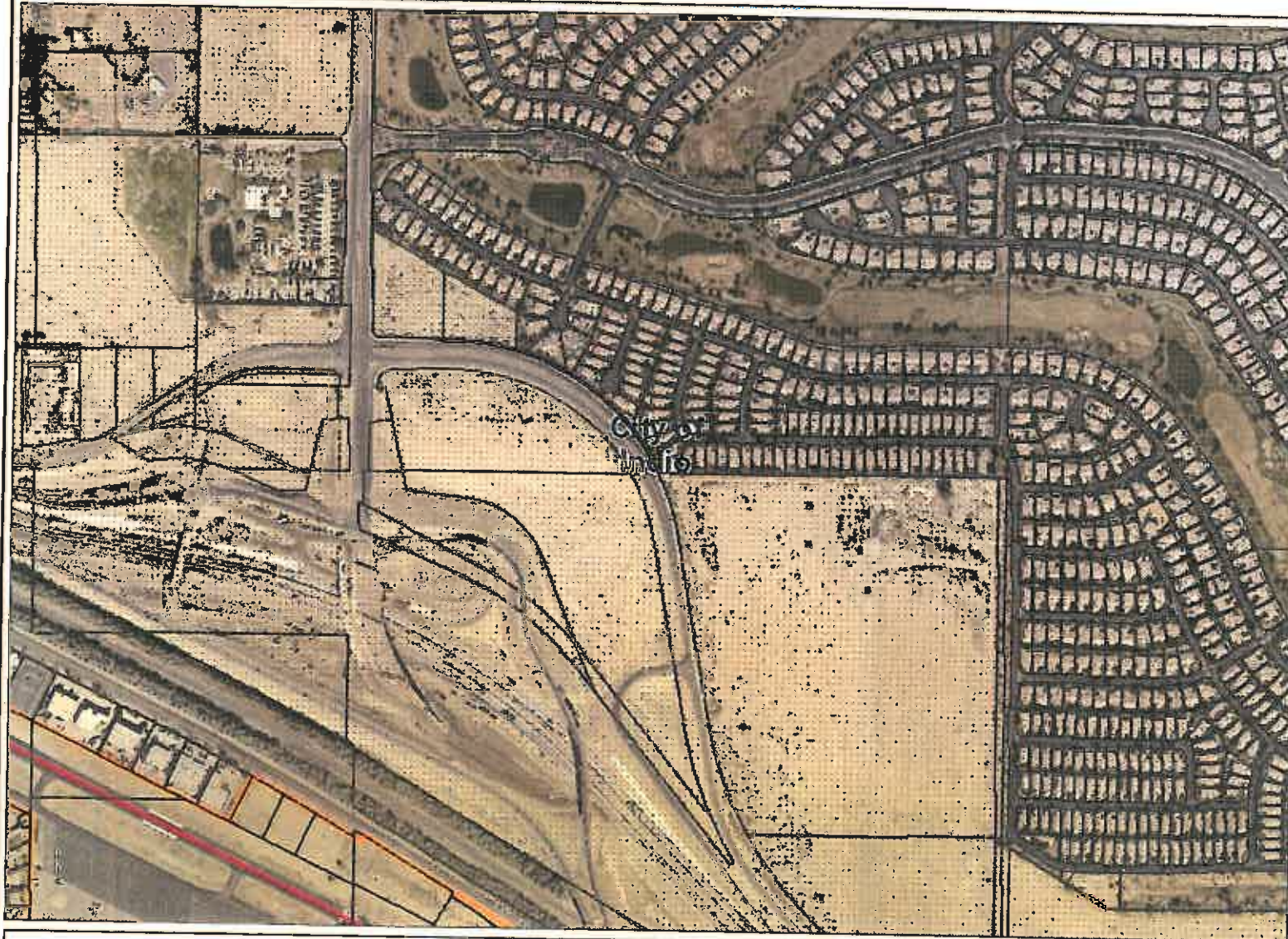
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Notes

Map My County Map



Legend

-  Parcels
-  Runways
-  Airports
-  Airport Influence Areas
-  Blueline Streams
-  Townships
-  City Areas
-  World Street Map



0 752 1,505 Feet

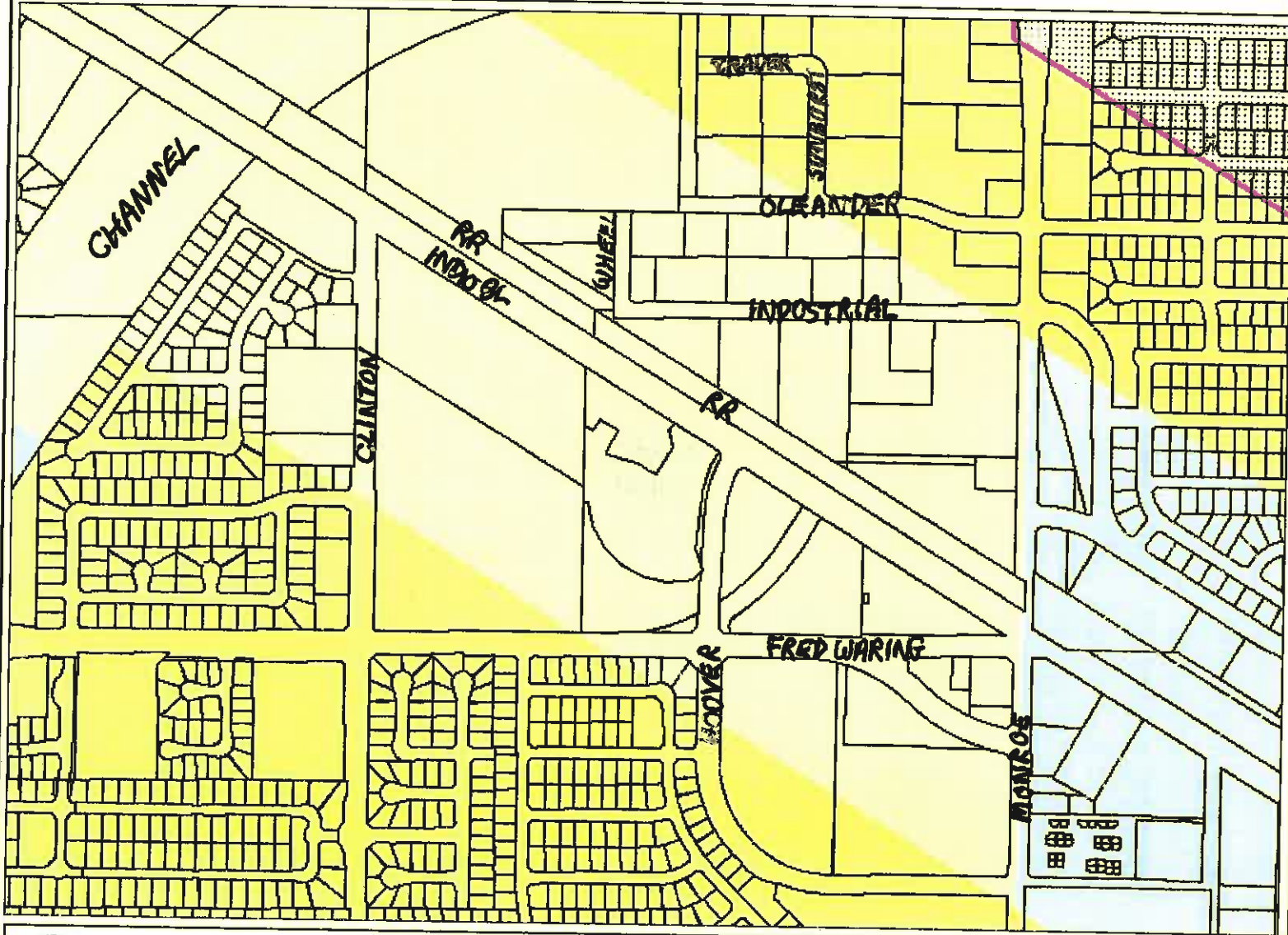
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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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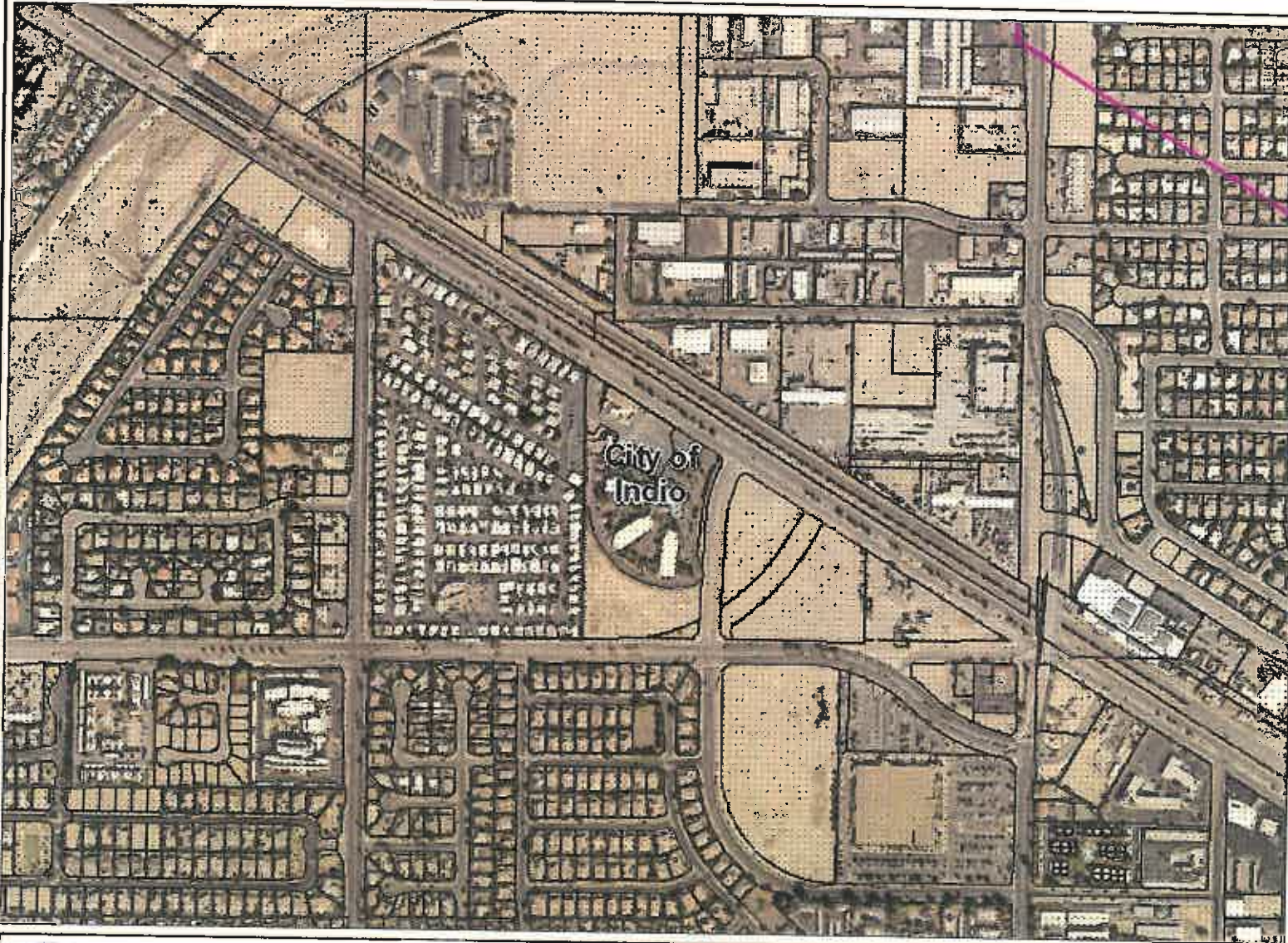


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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Blueline Streams
- Townships
- City Areas
- World Street Map



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0 752 1,505 Feet

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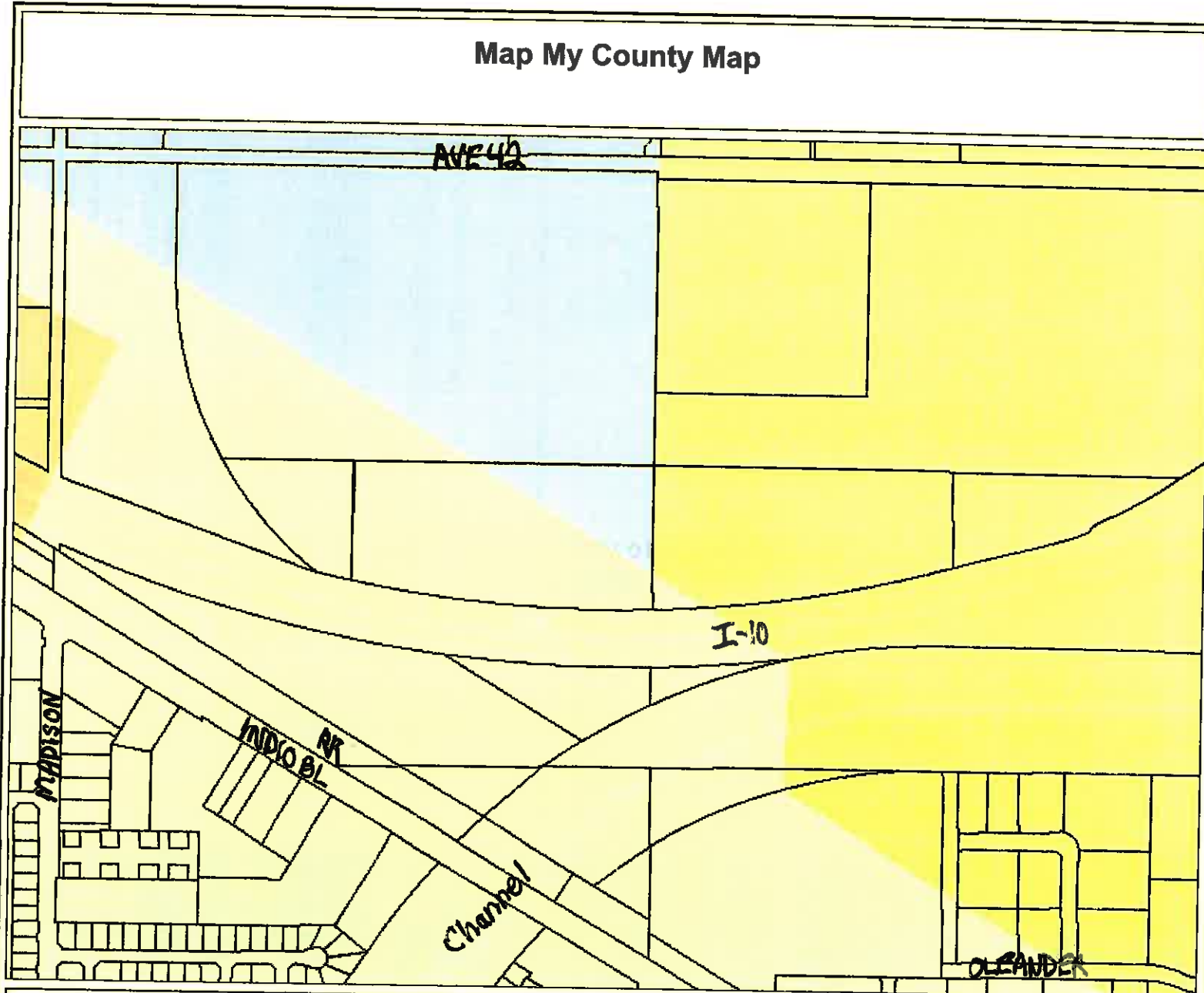
Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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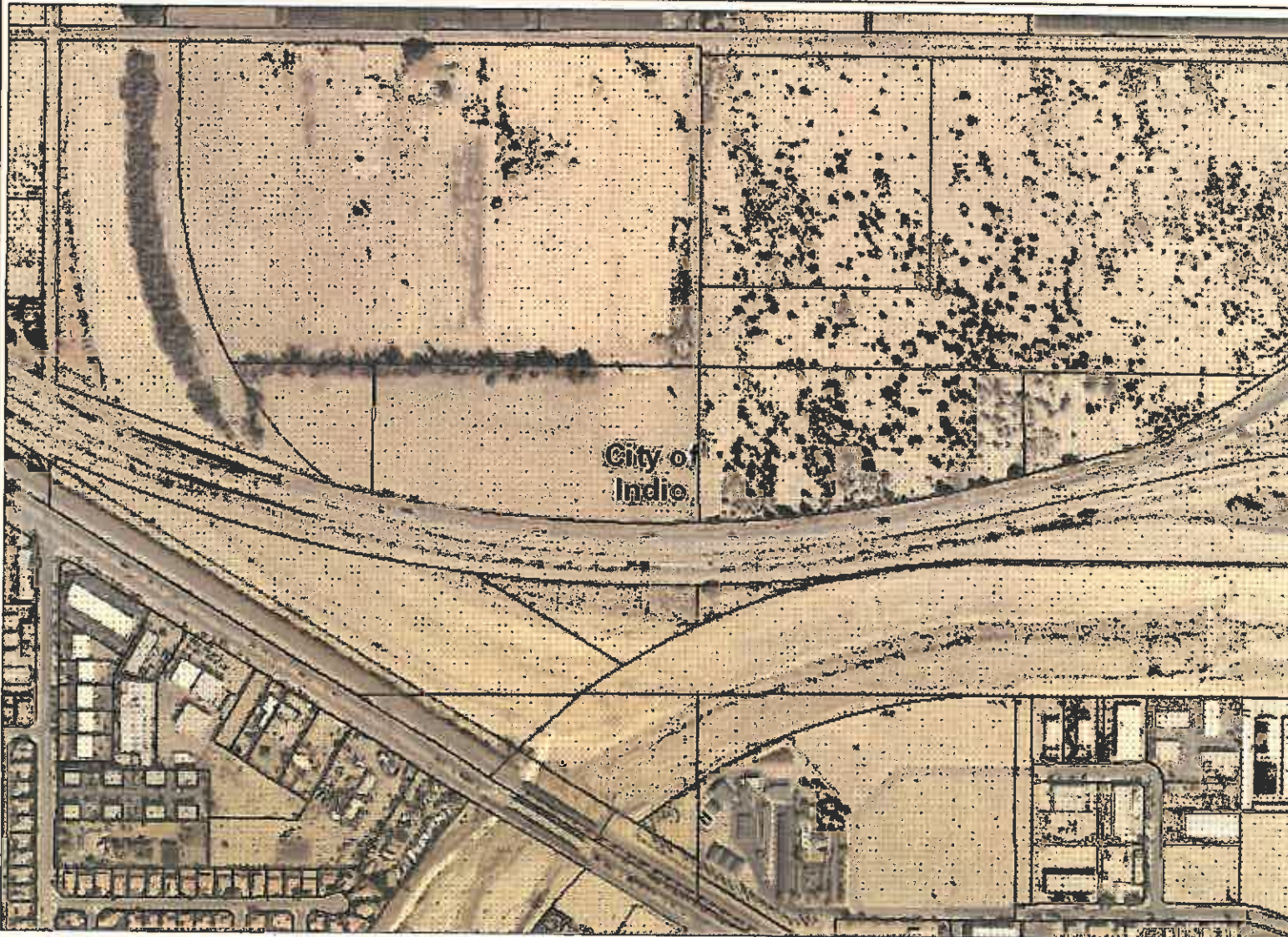
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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Blueline Streams
- Townships
- City Areas
- World Street Map



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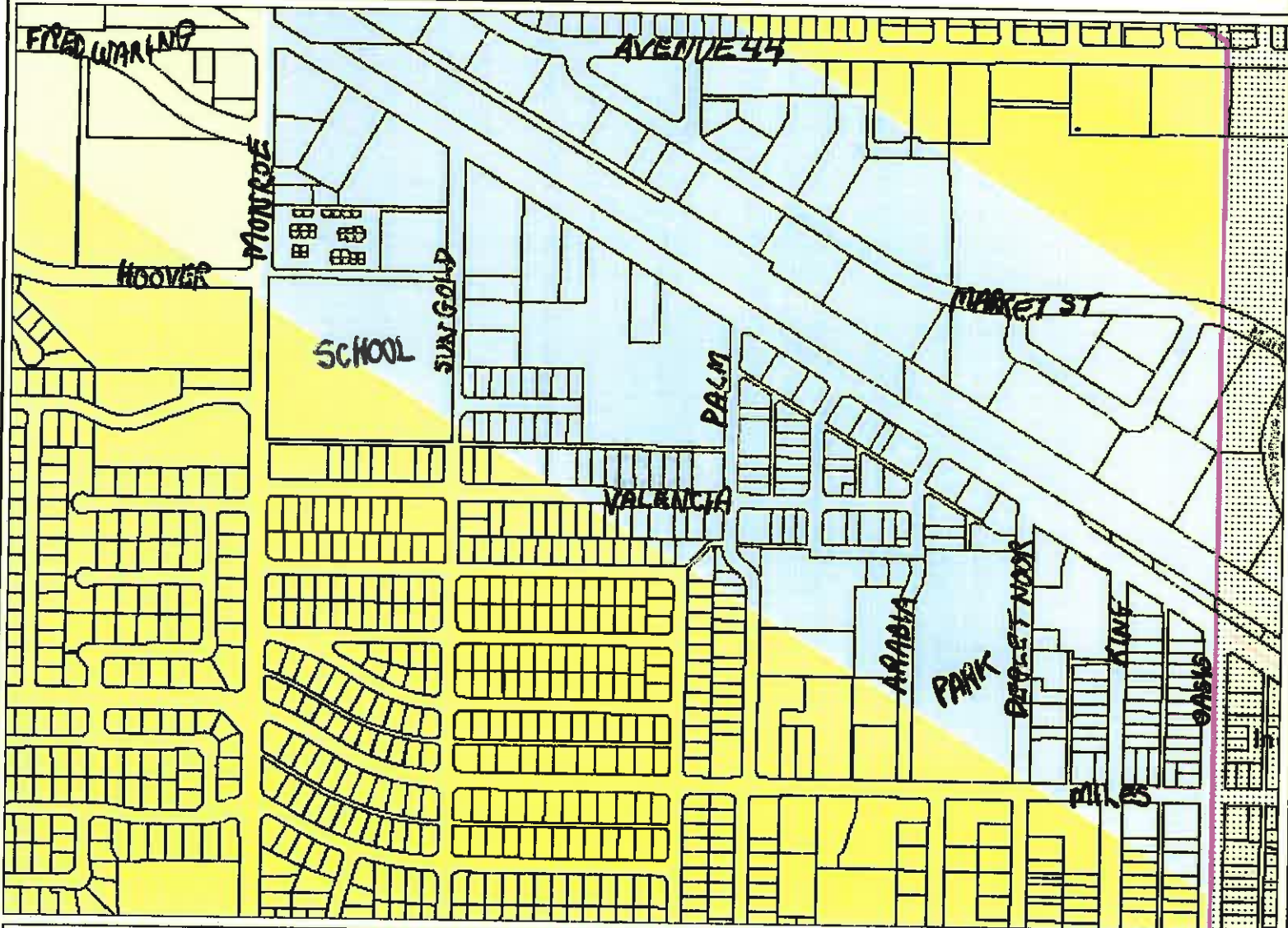


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Notes

Map My County Map



- Legend**
- Parcels
 - Runways
 - Airports
 - Airport Influence Areas
 - Airport Compatibility Zones**
 - OTHER COMPATIBILITY ZONE
 - A
 - A-EXC1
 - B1
 - B1-APZ I
 - B1-APZ I-EXC1
 - B1-APZ II
 - B1-APZ II-EXC1
 - B1-EXC1
 - B2
 - B2-EXC1
 - C
 - C1
 - C1-EXC1
 - C1-EXC3
 - C1-EXC4
 - C1-HIGHT
 - C2
 - C2-EXC1
 - C2-EXC2
 - C2-EXC3
 - C2-EXC5



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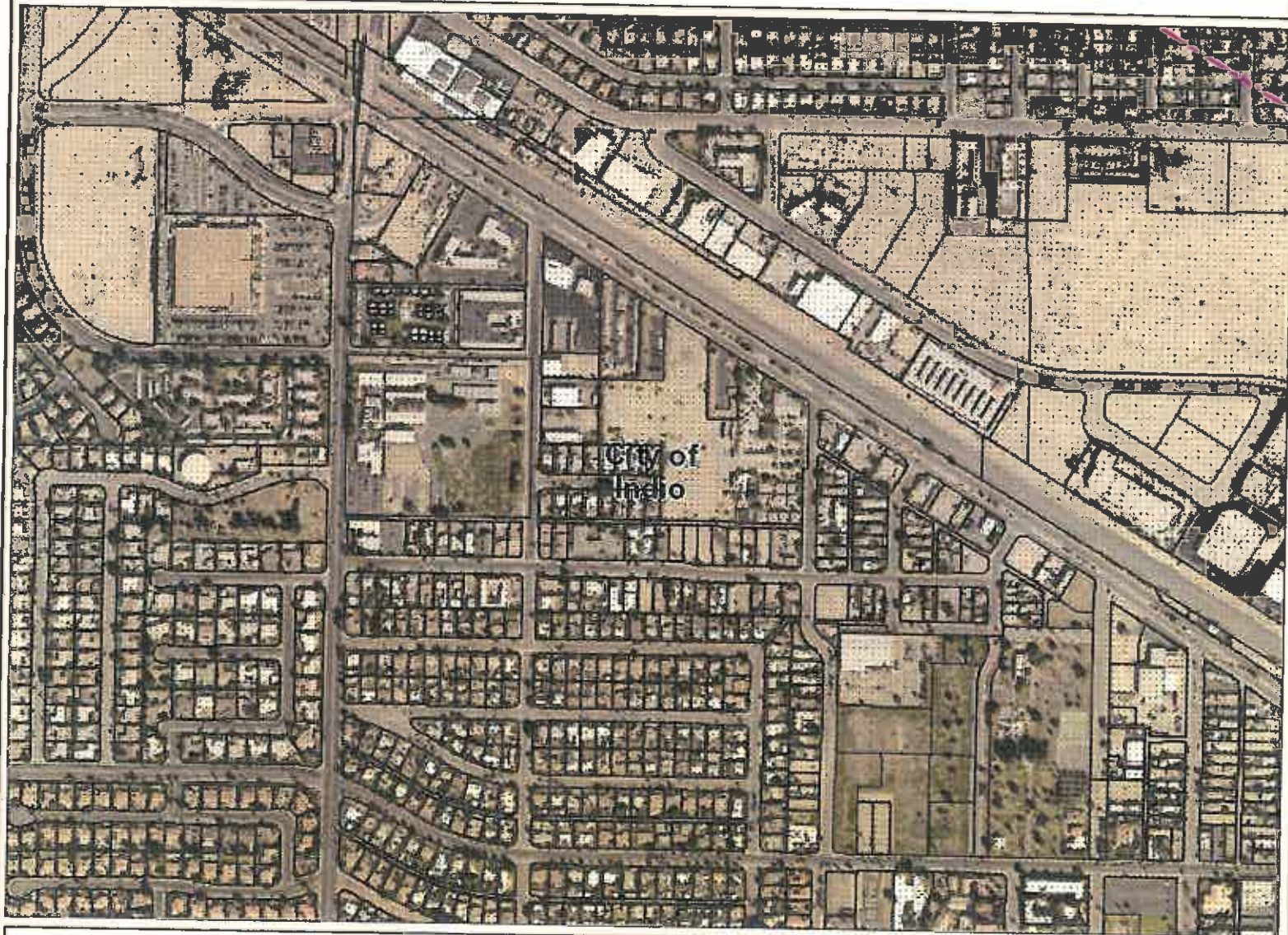


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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Blueline Streams
- Townships
- City Areas
- World Street Map



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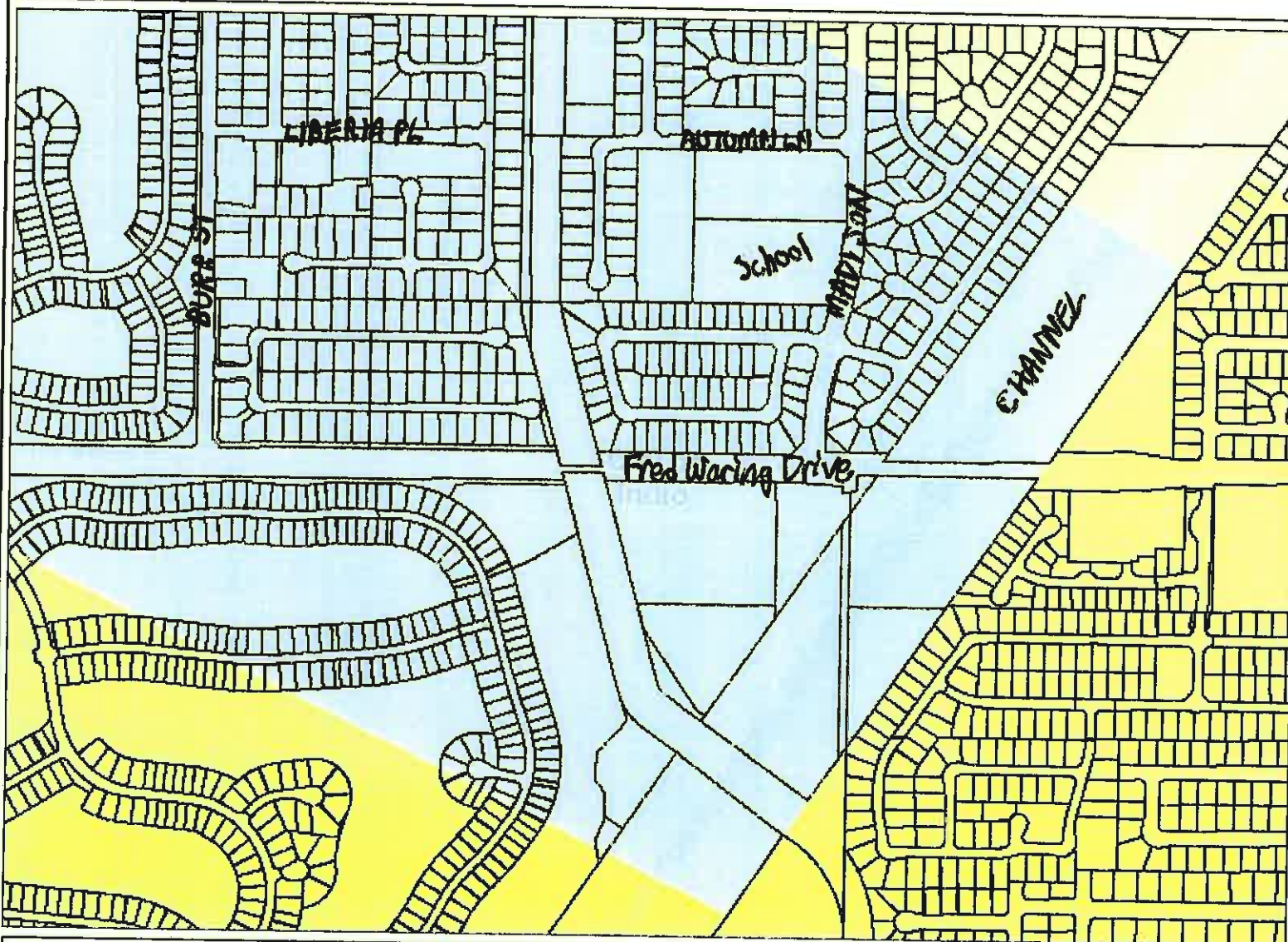


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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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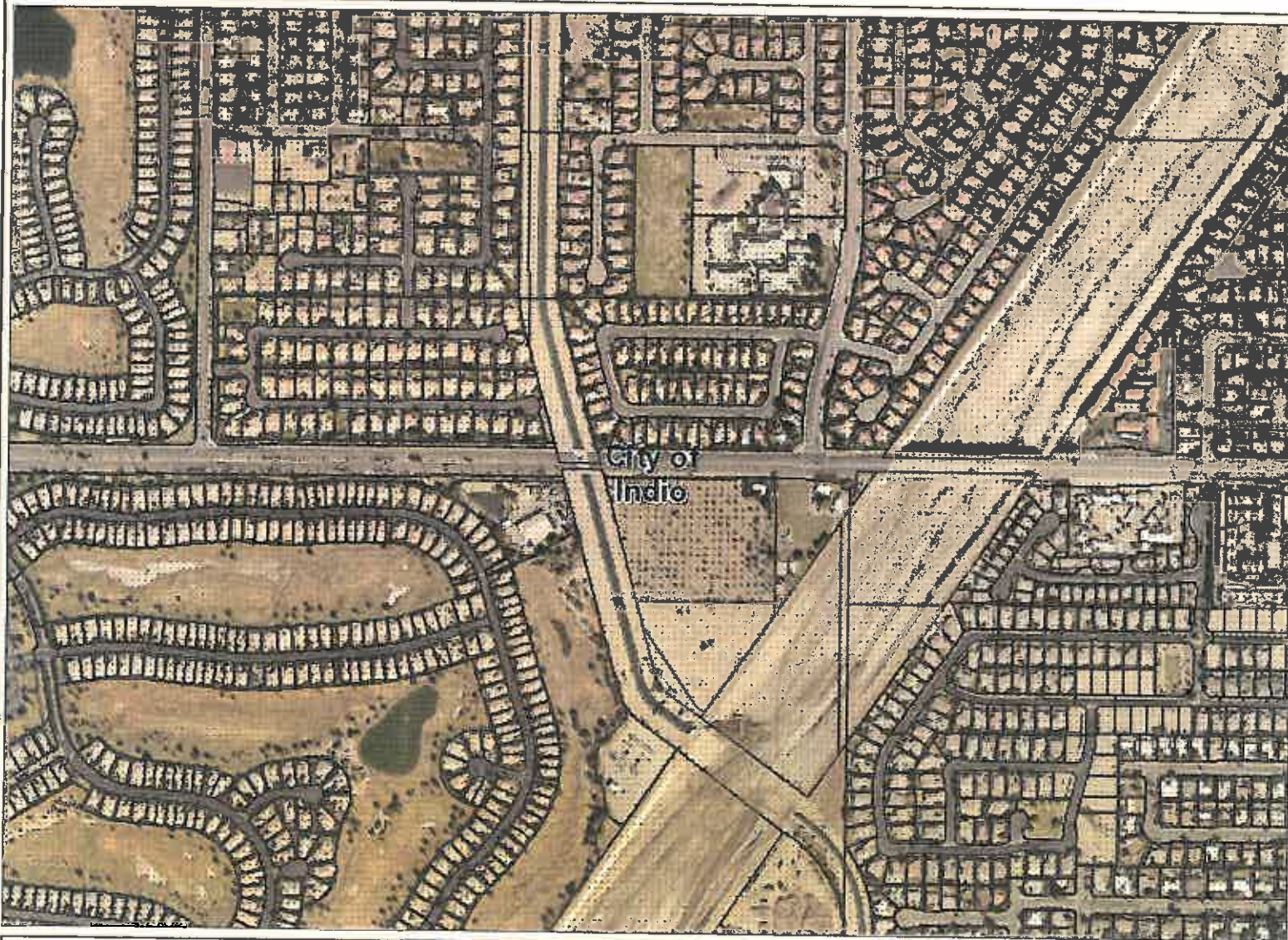


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Notes

Map My County Map



Legend

-  Parcels
-  Runways
-  Airports
-  Airport Influence Areas
-  Blueline Streams
-  Townships
-  City Areas
-  World Street Map



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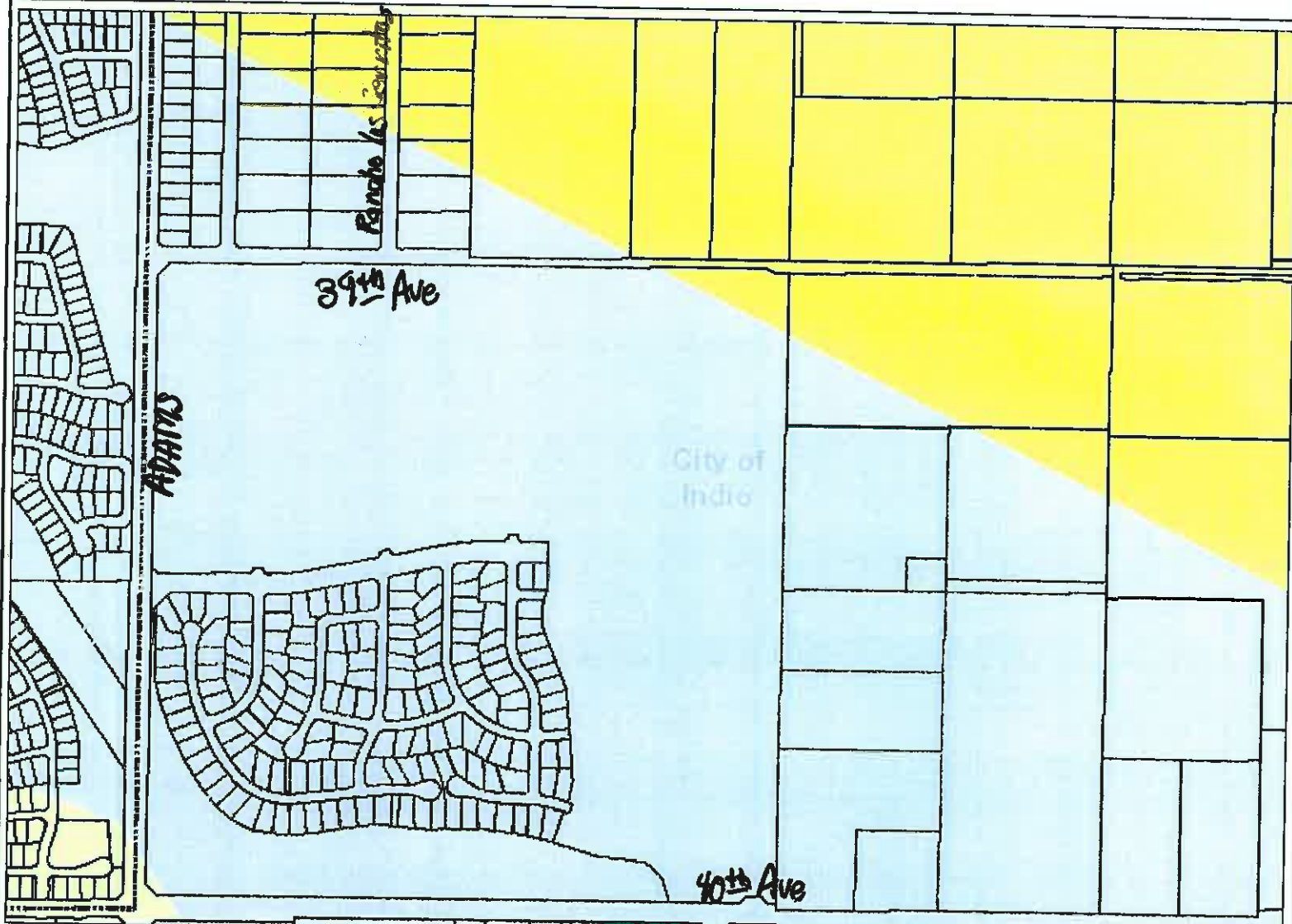
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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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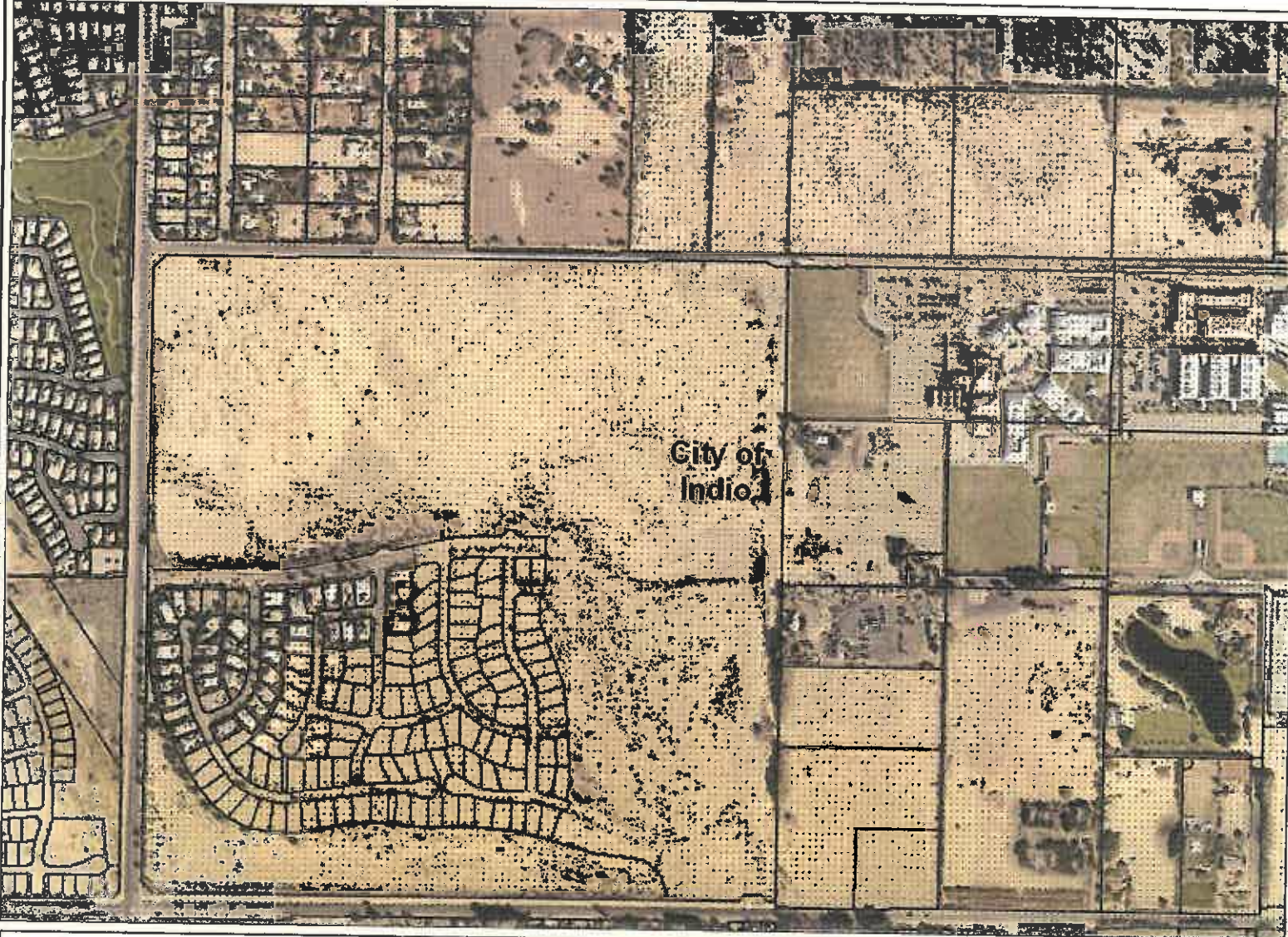


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Notes

Map My County Map



- Legend**
- Parcels
 - Runways
 - Airports
 - Airport Influence Areas
 - Blueline Streams
 - Townships
 - City Areas
 - World Street Map



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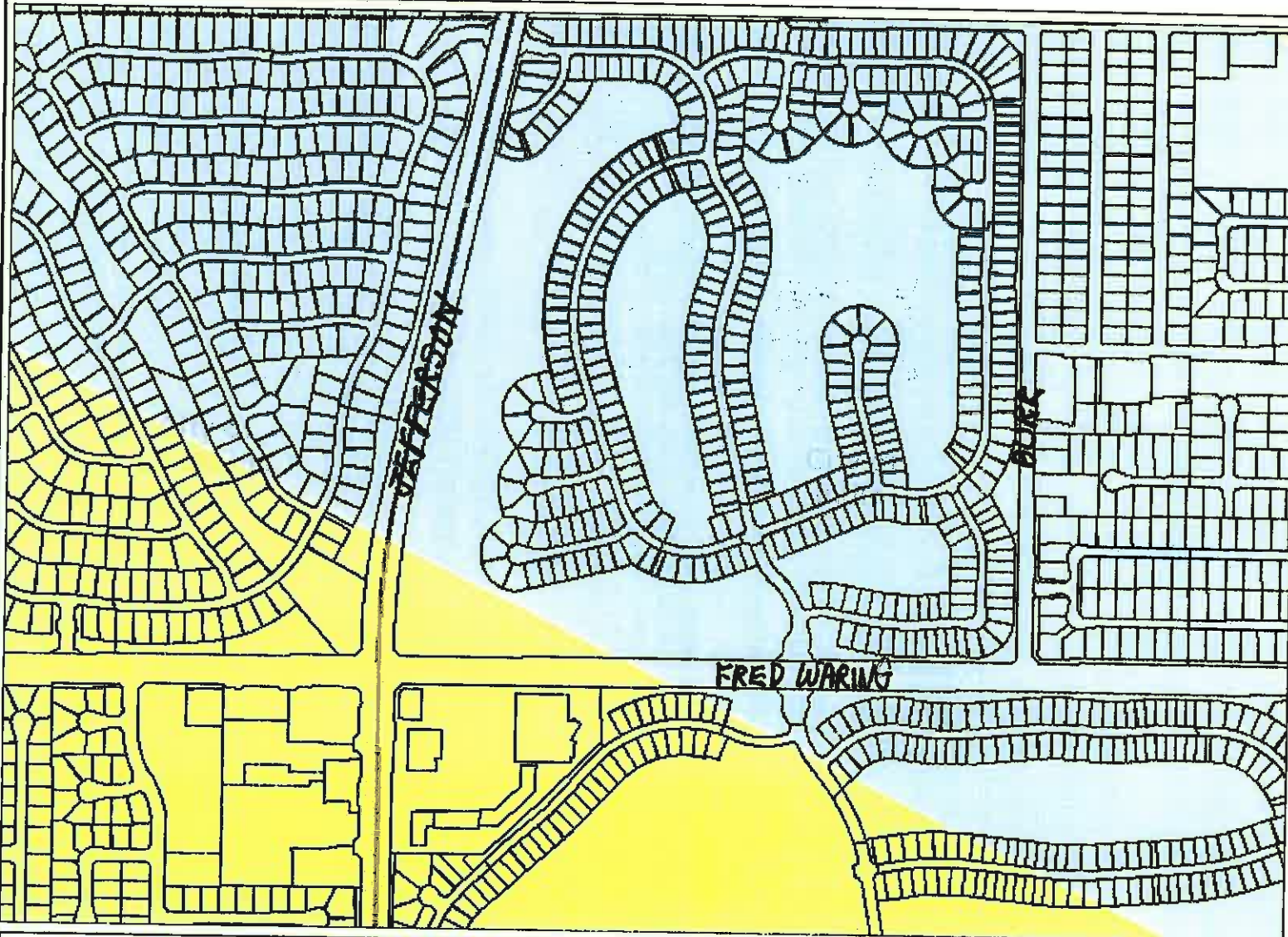


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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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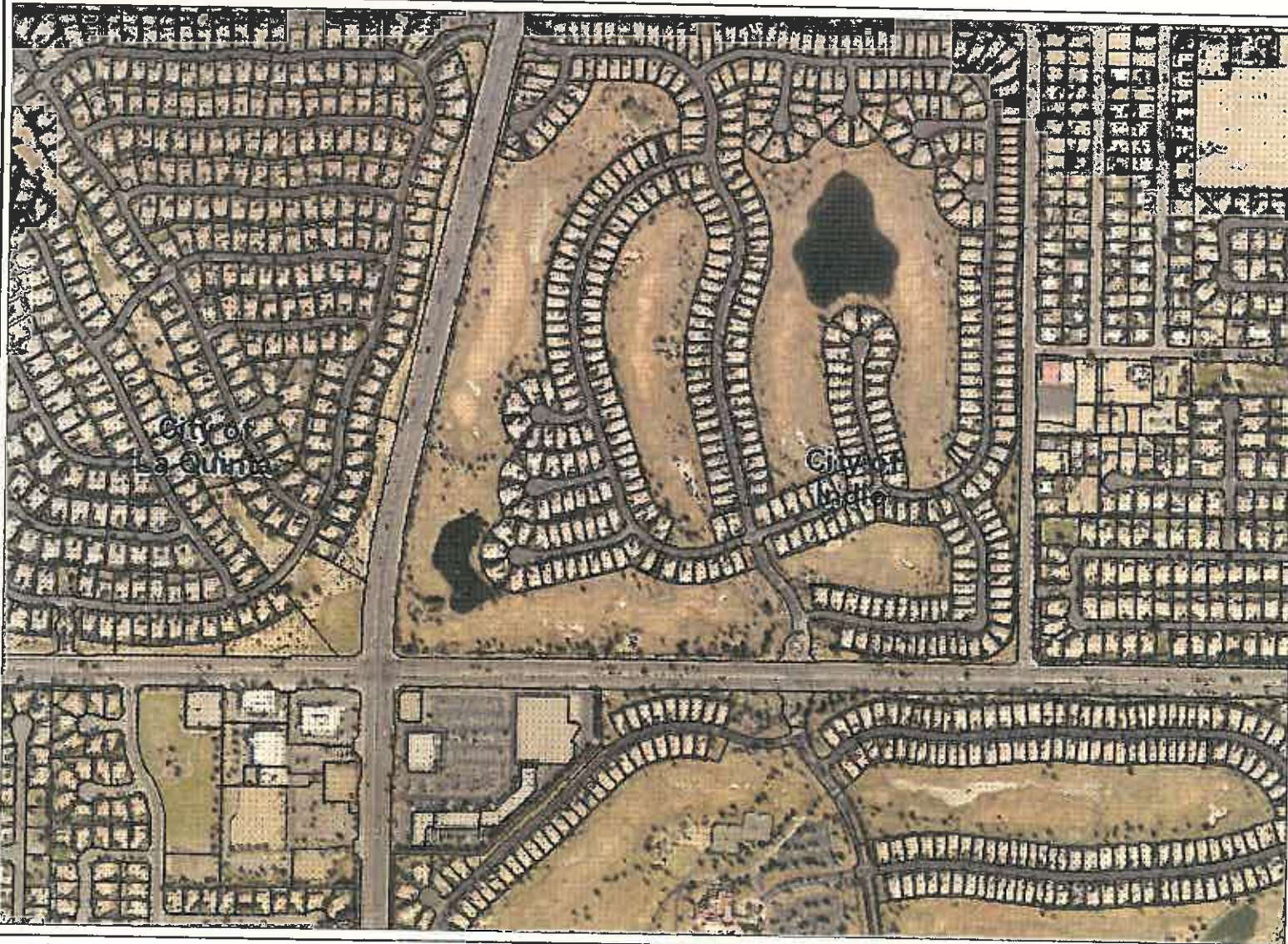
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Notes

Map My County Map



Legend

-  Parcels
-  Runways
-  Airports
-  Airport Influence Areas
-  Blueline Streams
-  Townships
-  City Areas
-  World Street Map



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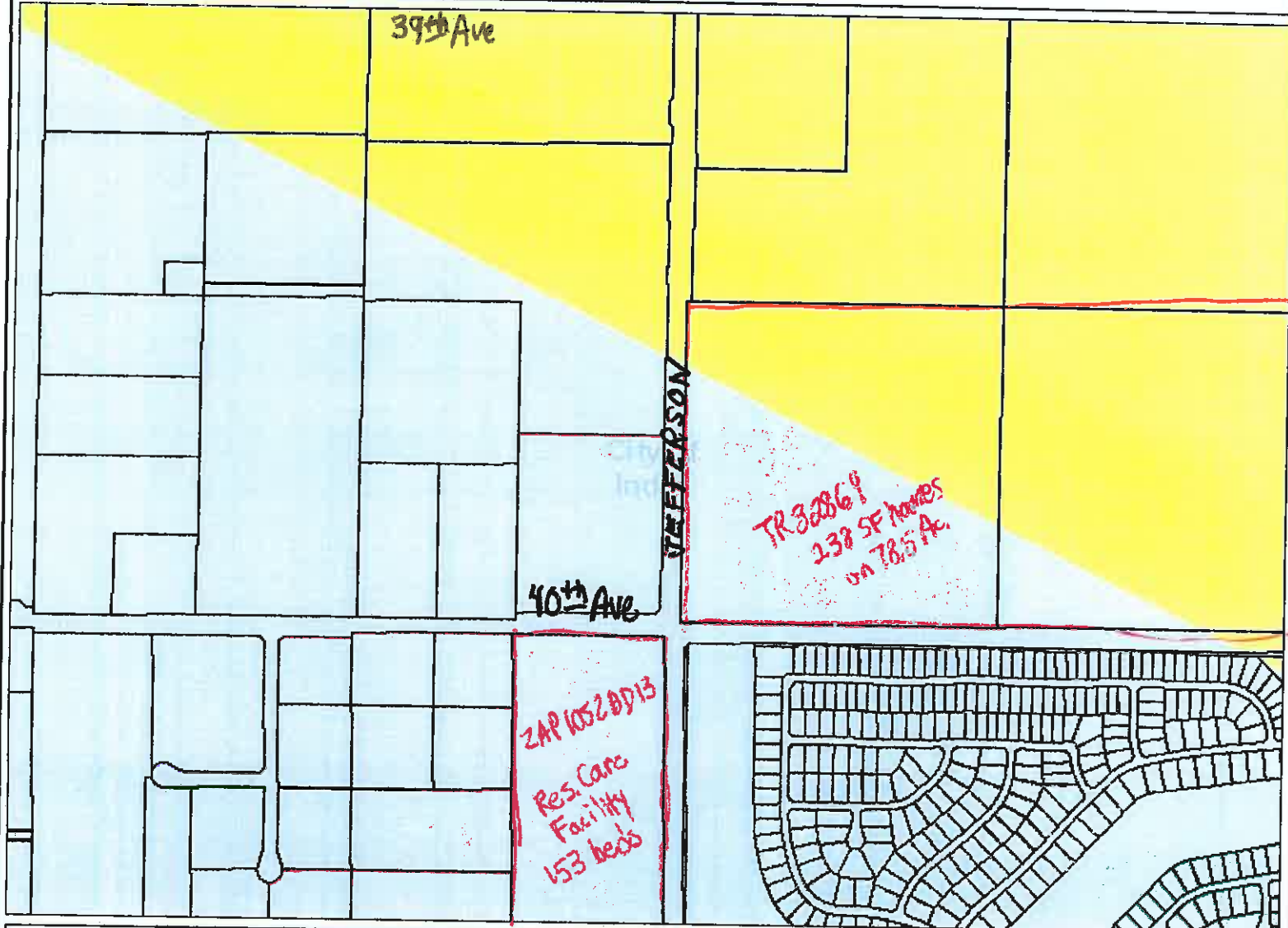
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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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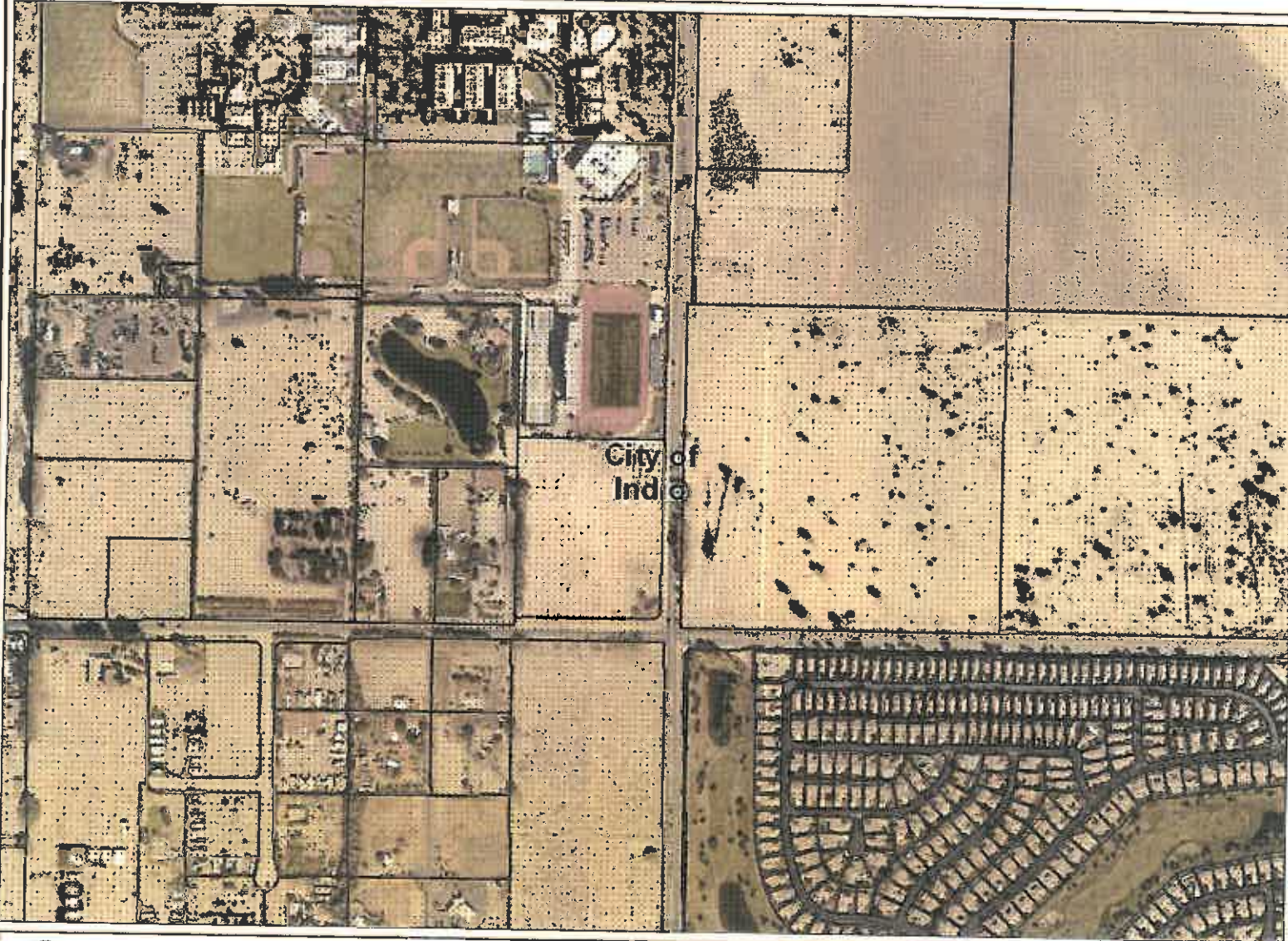
0 752 1,505 Feet

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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Blueline Streams
- Townships
- City Areas
- World Street Map



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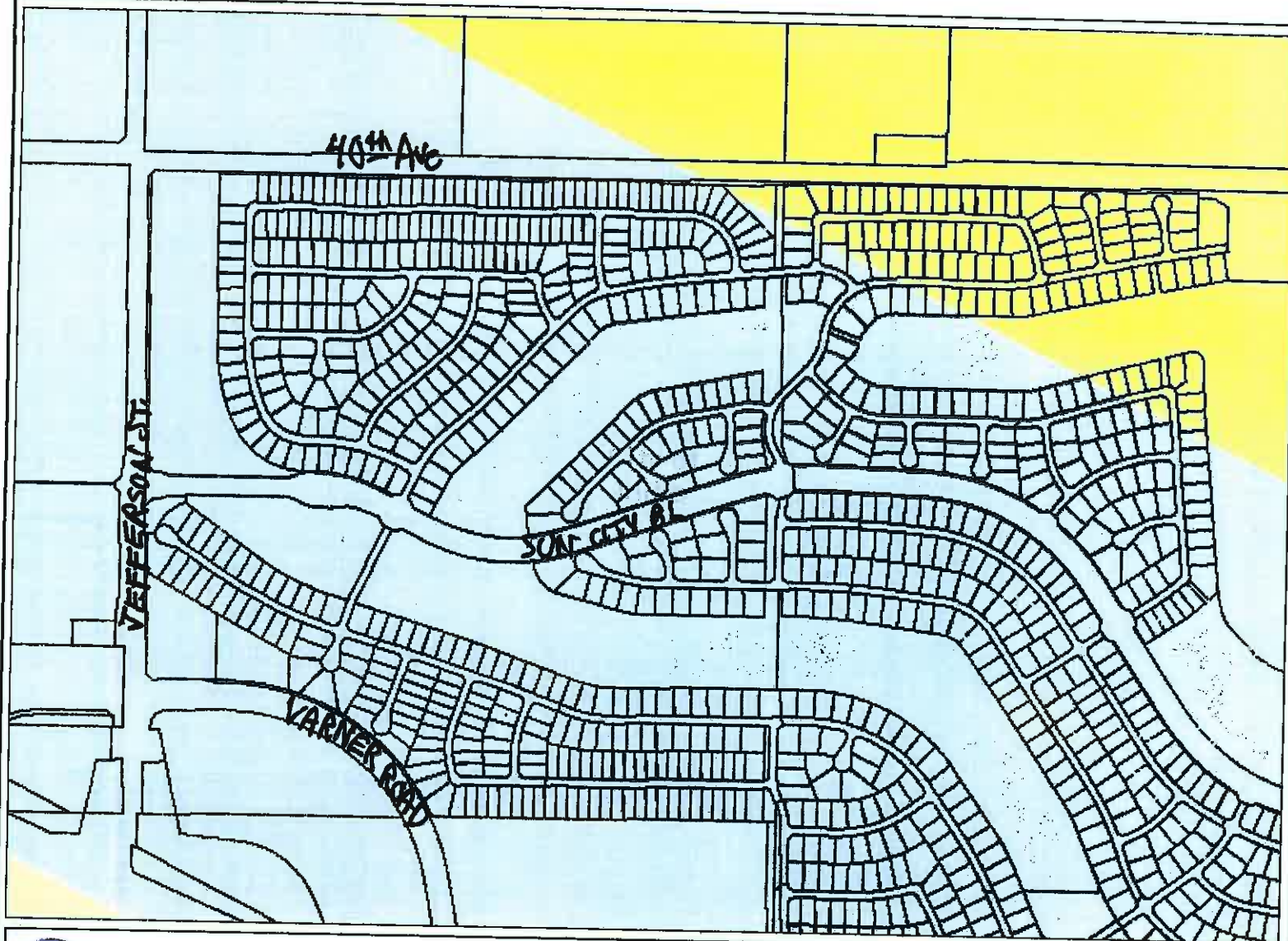
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Notes

Map My County Map



- ### Legend
- Parcels
 - Runways
 - Airports
 - Airport Influence Areas
 - Airport Compatibility Zones**
 - OTHER COMPATIBILITY ZONE
 - A
 - A-EXC1
 - B1
 - B1-APZ I
 - B1-APZ I-EXC1
 - B1-APZ II
 - B1-APZ II-EXC1
 - B1-EXC1
 - B2
 - B2-EXC1
 - C
 - C1
 - C1-EXC1
 - C1-EXC3
 - C1-EXC4
 - C1-HIGHT
 - C2
 - C2-EXC1
 - C2-EXC2
 - C2-EXC3
 - C2-EXC5



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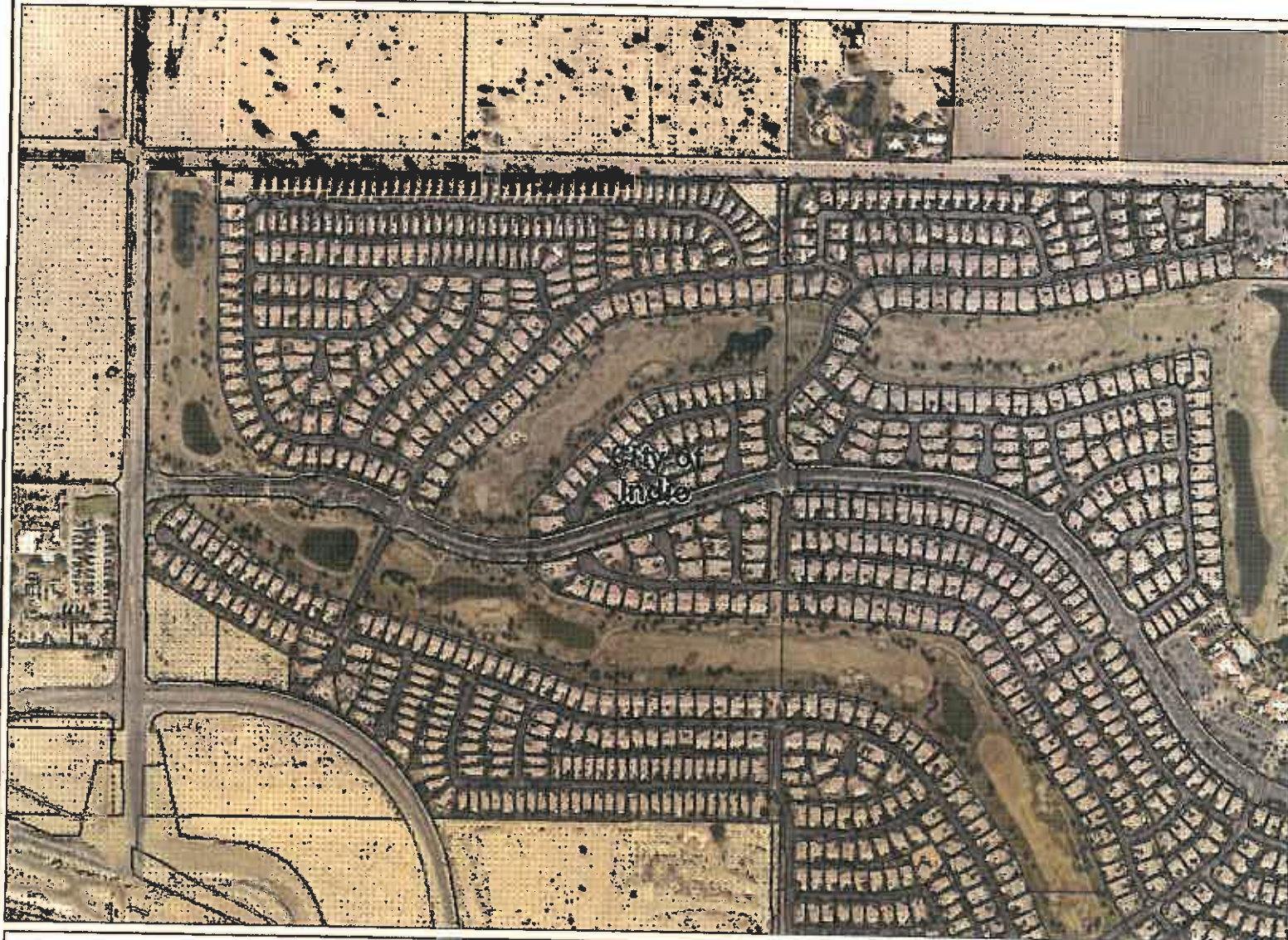


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Notes

Map My County Map



Legend

-  Parcels
-  Runways
-  Airports
-  Airport Influence Areas
-  Blueline Streams
-  Townships
-  City Areas
-  World Street Map



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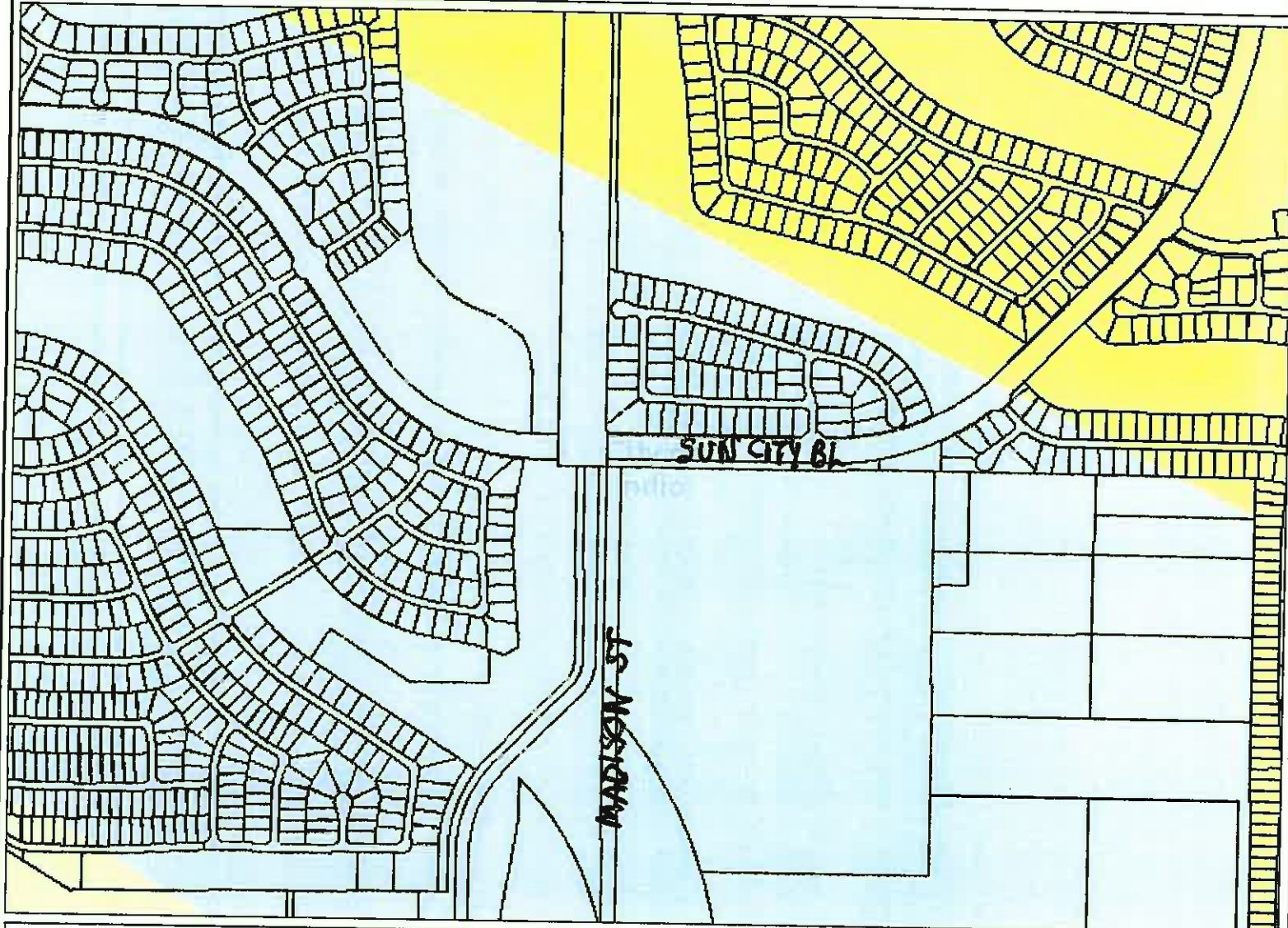


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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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Notes

Map My County Map



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- Parcels
- Runways
- Airports
- Airport Influence Areas
- Blueline Streams
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- World Street Map



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Notes

NOTICE OF PUBLIC HEARING

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Thursday from 9:00 a.m. to 5:00 p.m., except Monday, September 3 (Labor Day), and by prescheduled appointment on Friday, September 7, from 9:00 a.m. to 5:00 p.m.

ATTENTION: ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. The City of Indio will hold hearings on this project and should be contacted on non-ALUC issues.

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon St., 1st Floor Board Chambers
Riverside, California

DATE OF HEARING: September 13, 2018

TIME OF HEARING: 9:30 A.M.

CASE DESCRIPTION:

ZAP1075BD18 – City of Indio (Leila Namvar, Development Services Department). A proposal by the City of Indio to adopt a new General Plan 2040 to guide the future development of the City that focuses on revitalizing and connecting neighborhoods, establishing a human-scale network of complete streets and community open spaces, and enhancing community health and wellness. The General Plan includes the following elements/chapters: Vision and General Plan Strategies, Land Use and Urban Design, Mobility, Economic Development, Health and Equity, Parks, Recreation, and Open Space, Conservation, Infrastructure and Public Facilities, Safety, Noise, and Implementation. (The Housing Element is also part of the General Plan, but is not proposed for change as part of this effort.) The City includes land within all Compatibility Zones of the Bermuda Dunes Airport Influence Area.

FURTHER INFORMATION: Contact John Guerin at (951) 955-0982. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Ms. Leila Namvar of the City of Indio Development Services Department at (760) 541-4258.



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP1075BD18 DATE SUBMITTED: July 24, 2018

APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

Applicant	City of Indio	Phone Number 760-541-4258
Mailing Address	100 Civic Center Mall Indio, CA 92201	Email Lnamvar@indio.org
Representative	Leila Namvar	Phone Number 760-541-4258
Mailing Address	100 Civic Center Mall Indio, CA 92201	Email Lnamvar@indio.org
Property Owner	City of Indio	Phone Number
Mailing Address	Same as Above	Email

LOCAL JURISDICTION AGENCY

Local Agency Name	City of Indio	Phone Number 760-541-4258
Staff Contact	Leila Namvar	Email Lnamvar@indio.org
Mailing Address	100 Civic Center Mall Indio, CA 92201	Case Type
Local Agency Project No		<input checked="" type="checkbox"/> General Plan / Specific Plan Amendment <input type="checkbox"/> Zoning Ordinance Amendment <input type="checkbox"/> Subdivision Parcel Map / Tentative Tract <input type="checkbox"/> Use Permit <input type="checkbox"/> Site Plan Review/Plot Plan <input type="checkbox"/> Other

PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address	City Wide	
Assessor's Parcel No.		Gross Parcel Size
Subdivision Name		Nearest Airport and distance from Airport
Lot Number		

PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe)	This is a complete General Plan Update

Proposed Land Use (describe)	Please see attached Place Type Map	
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units) _____	
For Other Land Uses (See Appendix C)	Hours of Operation _____	
	Number of People on Site	Maximum Number _____
	Method of Calculation _____	
Height Data	Site Elevation (above mean sea level)	_____ ft.
	Height of buildings or structures (from the ground)	_____ ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight? <input type="checkbox"/> Yes <input type="checkbox"/> No	
	If yes, describe	This is a General Plan Update document. The City of Indio is within the Bermuda Dunes Airport Compatibility Plan.

- A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:**
1. Completed ALUC Application Form
 1. ALUC fee payment
 1. Plans Package (24x36 folded) (site plans, floor plans, building elevations, landscaping plans, grading plans, subdivision maps)
 1. Plans Package (8.5x11) (site plans, floor plans, building elevations, landscaping plans, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
 1. CD with digital files of the plans (pdf)
 1. Vicinity Map (8.5x11)
 1. Detailed project description
 1. Local jurisdiction project transmittal
 3. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
 3. Gummed address labels of all surrounding property owners within a 300 foot radius of the project site (**only required if the project is scheduled for a public hearing Commission meeting**). If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10) with ALUC return address. *

* Projects involving heliports/helicopter landing sites will require additional noticing procedures.



July 9, 2018
Development Services Department

RE: CITY OF INDIO GENERAL PLAN UPDATE – PUBLIC DRAFT

To Whom It May Concern,

The City of Indio is currently in the process of updating its General Plan. The General Plan is a state mandated document that represents goals, policies, and programs that provide the framework for the City's future physical development. The Indio General Plan will address land use, transportation, housing, safety, conservation, open space, noise, public health, sustainability, and economic development.

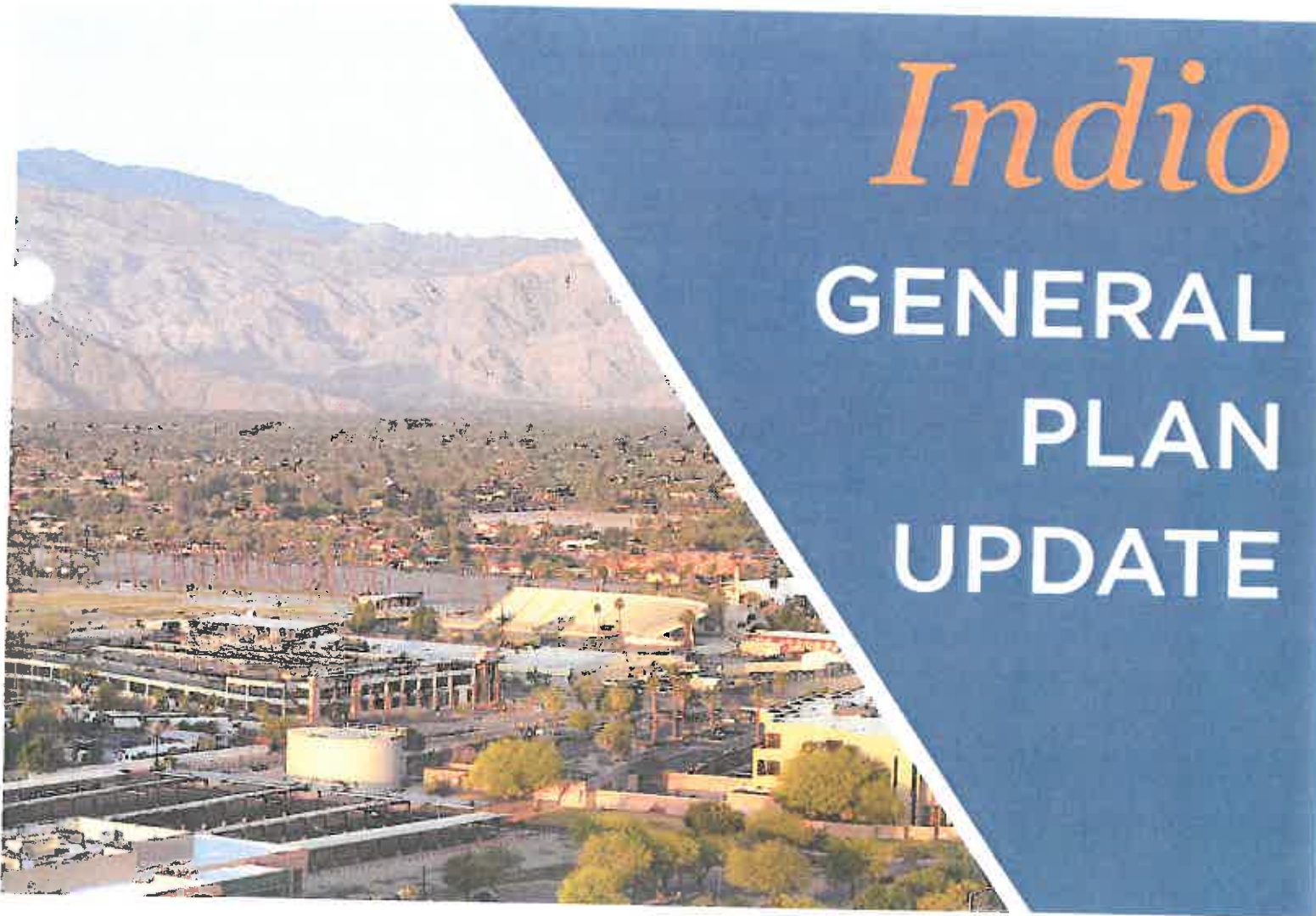
We are pleased to announce that a public draft of the Indio General Plan update is available for review. A copy of the draft General Plan is provided on the flash drive accompanying this letter. In addition, a print copy of the Place Type Map has also been provided. Please note that the draft General Plan can also be viewed and downloaded by visiting www.indio2040.org.

The above noted items are **draft** and intended only for your review and comment. Please provide your comments via email (lnamvar@indio.org) no later than Friday, August 31, 2018 before 5:00 pm.

Should you have any questions regarding the package material and/or the General Plan update process please do not hesitate to contact us.

Thank You,

Leila Namvar
Assistant Planner
760-541-4258



Indio

GENERAL PLAN UPDATE

Public Draft | June 2018



Public Draft

June 2018

Acknowledgments

CITY COUNCIL

Michael Wilson, Mayor
Troy Strange, Mayor Pro Tem
Elaine Holmes, Councilmember
Glenn Miller, Councilmember
Lupe Ramos Watson, Councilmember

PLANNING COMMISSION

Kenneth Weller, Chairperson
Eric Ceja, Vice Chairperson
Gloria Franz, Commissioner
Jacqueline Lopez, Commissioner
Nikko Alexander Ysiano, Commissioner

GENERAL PLAN ADVISORY COMMITTEE

Michael Wilson, Mayor
Lupe Ramos Watson, Councilmember
Eric Ceja, Planning Commissioner Vice Chairperson
Gloria Franz, Planning Commissioner
Stone James
Sean Smith
Patrick Blythe
Candelario Caldera

CONSULTANT TEAM

Raimi + Associates
Sargent Town Planning
Rincon Consultants, Inc.
Fehr & Peers
MR+E

CITY STAFF

City Manager

Mark Scott, City Manager

Development Services

Les Johnson, Development Services Director
Leila Namvar, Assistant Planner/Project Manager
Luis Cerros, Senior Plans Examiner
Gustavo Gomez, Planning Intern
Evelyn Beltran, Administrative Secretary

Public Works

Timothy Wassil, Public Works Director
Juan Raya, City Engineer
Tom Brohard, Traffic Engineer
Toyen Blake, Assistant Engineer
Sara Toyoda, Environmental Programs Coordinator

Police Department

Mike Washburn, Chief of Police
Chris Hamilton, Acting Assistant Chief
Benjamin Guitron, Administrative Officer
Erika Martinez, Administrative Services Manager
Sherri Van Dorn, Executive Assistant to the Chief of Police

Fire Department

Captain Dale Frailey, Fire Marshal
Kris D. Gleeson, Assistant Fire Marshal
Emergency Services
Michelle Kelly, Emergency Services Coordinator
Addison Hesslink, Emergency Services Coordinator

Community Services

Jim Curtis, Community Services Manager
Rosalba Campos, Community Program Administrator

Finance Department

Rob Rockwell, Finance Department Director and Assistant City Manager
Veronica Alvarado, Fiscal Officer

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CHAPTER 2 INTRODUCTION

CHAPTER 3 LAND USE AND URBAN DESIGN

CHAPTER 4 MOBILITY

CHAPTER 5 ECONOMIC DEVELOPMENT

CHAPTER 6 HEALTH AND EQUITY

CHAPTER 7 PARKS, RECREATION, AND OPEN SPACE

CHAPTER 8 CONSERVATION

CHAPTER 9 INFRASTRUCTURE AND PUBLIC FACILITIES

CHAPTER 10 SAFETY

CHAPTER 11 NOISE

CHAPTER 12 IMPLEMENTATION

...housing
...ver tipos de viv...



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chapter one

VISION AND GENERAL PLAN STRATEGIES

This General Plan presents our vision for Indio over the next 20 years. This Plan is the culmination of a multi-year community-wide effort to reflect and define who we want to be as a community. We seek to enhance our local economy, maintain our “City of Festivals” reputation, while simultaneously improving our access to quality education, housing, and jobs. This General Plan Update is an important planning document that implements the community’s vision, establishing goals, policies, and implementation actions that will help us achieve our long-term vision as a community that values our unique culture, entertainment and arts, and local character. This is our plan for our future. We take great pride in this document, and we are committed to achieving our collective community vision.

Vision Statement

“Indio is recognized as a vibrant community for arts and entertainment, education, commerce, and culture.”

Being the oldest City in the Coachella Valley, Indio has gone through significant transformation throughout its history. Indio was the heart of commerce and activity in the Coachella Valley, a hub of tourism, manufacturing, retail and services, and agriculture. Over the last 60 years, as automobile use became more widespread and commercial activity in the Coachella Valley shifted from agriculture and military activity to tourism and recreation, the centers of activity shifted from Indio to cities on the western end of the Coachella Valley. Recently, Indio has begun to experience a resurgence in activity and investment. New, high-quality neighborhoods have been built, and new internationally-acclaimed music festivals have been established.

The General Plan provides a blueprint for Indio to build on this momentum and to become a complete 21st century city. The City will move into the next phase of its history, upgrading and modernizing itself for a future where connectedness and human-scale places are the keys to a vibrant, successful community.

Key Outcomes

Key outcomes of this General Plan include:

- + **Quality of Life:** A high quality of life for all of our residents.
- + **Night Life, Entertainment, and Recreation:** A lively Downtown Indio, exceptional city-wide events, and regional parks and trails that will attract visitors and residents alike.
- + **Multi-Modal Transportation Network:** An interconnected transportation network that serves all users and modes in a healthy, equitable manner.
- + **Sustainable Community:** An efficient community that can persist for generations.
- + **Range of Housing Options:** A wide variety of housing types to serve a broad and diverse community of new and existing residents, providing housing opportunities for households of all ages, types, incomes, and lifestyles.
- + **Exceptional Educational Opportunities:** Extensive educational and vocational training opportunities that help develop a diverse and well-trained workforce.
- + **Expanded Employment:** A strong, resilient economy that offers opportunities for entry level, service, technology, and entrepreneurial employment to meet the needs of Indio’s residents and to attract future residents to the region.

- + **City of Festivals:** Indio’s internationally-known festivals will continue to attract and support entertainment and hospitality that enhance Indio as the City of Festivals.
- + **Compelling Retail and Commercial Uses:** A retail sector that fully serves the needs of all of the City’s residents, offering both quality every-day and specialty retail uses at locations throughout the City.
- + **Efficient Use of Infrastructure:** A well-planned and smartly-developed City that grows in concert with its ability to provide services.

The Strategies to Achieve Our Vision

To implement the vision and key outcomes, the General Plan includes a series of major strategies to perform over the next 20 years.

Restore Downtown as the heart of the community. Indio is one of a few cities in Southern California’s desert region that has a historic downtown. Like many California cities, Indio’s Downtown suffered disinvestment as new commercial centers and neighborhoods were built on the periphery of the City. A key effort of the City will be to attract new development to the Downtown. In large part, this will be guided by a new Downtown Specific Plan and placemaking strategies. The City will also work to revitalize and connect the surrounding neighborhoods to Downtown, restoring the Downtown and its surrounding neighborhoods as the heart of Indio. The City will focus staff time and resource and public infrastructure investments in the area to help attract private investment.

Reinvent the Highway 111 Corridor. The area around Monroe Street and Highway 111 was the City’s second generation commercial center, following the Downtown as the center of commerce in Indio. This area of the City has lost much of its vitality and economic activity. Uniquely located near the Downtown and at the cross-roads of Jefferson, Monroe, and Jackson Streets, the Highway 111 corridor serves as a major connector to the rest of the Coachella Valley. Highway 111 provides local access to homes and businesses while facilitating regional travel. This General Plan lays out a new future for the Highway 111 corridor centered on creating a walkable, mixed use-district in the vicinity of the old Fashion Mall. This “Midtown District” can serve as both a center of commerce and a gateway to the City’s festivals.

Establish a human-scale network of complete streets and community open spaces. Indio residents articulated two desires that are mutually reinforcing: to provide more opportunity for active transportation, and to improve the visual character of the community. Both of these goals can be accomplished through urban design that focuses on the creation of a human-scaled public spaces. By designing public spaces, such as streets, sidewalks, parks, and other community spaces, with an emphasis on people first, the City’s centers, neighborhoods, and districts will support more opportunities to gather and interact, reinforcing Indio’s sense of community.

The City’s centers, neighborhoods, and districts will be far more valuable and sustainable when connected by a complete circulation network that effectively balances all modes of transportation and supports all users. Designing the circulation network for pedestrians and bicyclists will result in places and streets that are more comfortable, safer, and easier to use.

Indio residents also expressed a desire for attractive, well-landscaped spaces that support a variety of family - and community-recreation activities. Quality of life, general health, and community character will

all be enhanced by new parks and recreational facilities and the rehabilitation of existing facilities that better support active, outdoor-oriented lifestyles.

Expanding commerce in the City. Indio expressed a deep desire for enhanced shopping and increased employment opportunities within the City. The City's central location in the Coachella Valley and regional freeway and rail accessibility will enable Indio to continue to attract and maintain a wide variety of commercial, office, and industrial employers within the Workplace and Employment District, Regional Commercial District, and centers. The City will build upon and expand these existing employment areas to generate more and new types of jobs in the future. The City will also capture neighborhood- and community-serving retail and services. Together these will improve the quality of life, diversity and resiliency of the local economy.

Strengthen tourism and hospitality sector. Indio's year-round festivals are the envy of cities everywhere. The variety of entertainment activities draws people from around the world to Indio, bringing more than 100,000 people into the City for just a single event. Yet, most visitors do not stay, shop, or dine in the City, creating a missed opportunity to capture revenue and taxes locally. The City seeks to enhance its hospitality offerings, emphasizing high-quality, resort-oriented development around the Festival District / Polo Grounds to both leverage and support the unique amenity.

Revitalize and connect neighborhoods. Indio is known throughout the Coachella Valley for its excellent new neighborhoods. These modern neighborhoods are regarded as models for high-quality, suburban development. Yet the City has a range of other traditional neighborhood types too. Many of these neighborhoods have suffered disinvestment over time. As the City moves forward with implementing this General Plan, a part of its success will hinge on the ability to revitalize existing neighborhoods through public improvements that spur private investment. The City will also create new Connected Neighborhoods anchored by Neighborhood Centers with schools, retail and services, and other amenities that enhance the quality of life in these neighborhoods. Similarly, new Desert Estate neighborhoods have the opportunity to capitalize on their unique desert context by emphasizing their rural location and character.

Enhance community health and wellness for everyone in Indio. A healthy community enables people of all ages and abilities to lead full, productive, and enjoyable lives. Indio is committed to promoting the well-being of its residents. The General Plan outlines steps that incorporate opportunities for active living into every neighborhood, maintain a clean and healthy environment, provide access to quality health and mental care services and facilities, provide decent and affordable housing, increase access to healthy and affordable foods, and nurture quality educational and employment opportunities.

Community Engagement

Through a multi-year process, the City of Indio has worked with community members, businesses, and local organizations. The General Plan Update community engagement process occurred in two phases, the first from 2014 to 2016 that focused on public opinion and visioning. The City created a community questionnaire, hosted pop up workshops, and held a student essay contest. For more information on phase one, see Appendix A.

The second phase of engagement occurred from 2017 to 2018. During this time, the City conducted stakeholder interviews, convened a General Plan Advisory Committee (four meetings), hosted a community workshop, and presented to City Council and Planning Commission. The workshop took place in September 2017 with over 80 community members and interested stakeholders in attendance. The goal of this workshop was to confirm the direction and priorities for the General Plan. The top three priorities expressed by the community were to restore Downtown Indio, revitalize Highway 111, and balance multi-modal transportation.



Participants at the September 19th 2017 General Plan Workshop at the Indio Senior Center

Old Time

INDIO

*Calif is beautiful
I will be in
Riverside tomorrow
weather is nice
I am coming & bringing*

INDIO
JAN 10
1949
CALIF

Mr. Joe Lyon





chapter two

INTRODUCTION

This updated General Plan represents a blueprint for the future of Indio and its residents. In preparing for the challenges and opportunities ahead, Indio has developed a General Plan that reflects the unique local identity, generation of new economic opportunities, and desire to continue lively festivals and preserve the desert habitat that extends beyond the City's boundaries. Economic investment, Downtown revitalization, and sustainability are centrally embedded in the General Plan goals, policies, and actions. This General Plan Update is partially funded by a Southern California Association of Governments (SCAG) Grant.

City of Indio Profile

Regional Context

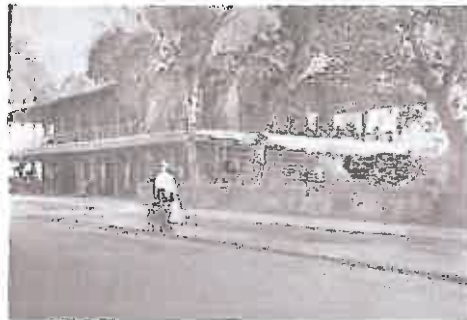
Indio is located in Riverside County, approximately 120 miles east of Los Angeles and 20 miles east of Palm Springs (refer to Figure 2-1, Regional Location). It is bordered by the City of La Quinta to the west, unincorporated Riverside County land to the south and to the north, and the City of Coachella to the east. Indio's General Plan encompasses a planning area that includes the incorporated City limits as well as the sphere of influence (refer to Figure 2-2).

City History

Indio is the oldest City within the Coachella Valley. Surveyed in 1872, it was chosen as a suitable location for a railroad depot because of its location halfway between Yuma, Arizona and Los Angeles, California. Originally named Indian Wells, the town was renamed Indio (Spanish for "Indian") to avoid confusion with other localities.¹ By 1876, the Southern Pacific Railroad began running trains from Los Angeles to Indio and the route was extended to Yuma the following year. In 1888, a formal Indio town site was surveyed and the plat was filed with the San Diego County Recorder. In 1893, Indio became one of the twelve judicial townships in the newly designated Riverside County. Most of the early settlement was by families attracted by the "Desert Entry" Homestead Act. Artesian wells and other available water sources likely influenced the placement of these homesteads. Early cultivated crops included melons, vegetables, and date palms. Date palms from Algeria were sent to the region in 1891.

Tourism played an important role in Indio's 20th Century growth. In 1901, the Riverside Press reported that Coachella Valley amusements consisted of "tennis, croquet, baseball, mountain climbing, and trails along the desert." In 1903, a tent health camp was opened just to the west of the Indio railroad depot, to take advantage of the highly-publicized health benefits of the desert. Golf courses were established in the region as early as 1926 and remain a tourist draw. Indio became an incorporated City on May 16, 1930.

In the early 1940s, Indio's business district consisted of more than 125 businesses, including the Indio branch of the Bank of America, two weekly newspapers, a movie theater, two hospitals, five hotels, auto camps, two department stores, and grocery stores.



Indio Train Station circa 1905-1907. Courtesy of CVWD.



Railroad worker cottages circa 1907. Courtesy of CVWD.

¹ Nordland, Ole. "Indian Wells." *The Periscope, A Collection of Stories and Recollections about the Coachella Valley* (1978).

In 1942, the U.S. War Department ordered Major General George Smith Patton, Jr., U.S. Army, to create, equip, and command a desert training center in California to prepare troops to fight in North Africa. General Patton's Desert Training Center was located near the present junction of I-10 and State Route 195. This desert training center became the largest Army post in the world in 1942. Indio local celebrity, Jacqueline Cochran, was named Director of Women Pilots of the U.S. Army Air Force. She trained women flyers to ferry Hudson bombers between the United States and England.

In the late 1950s, Indio was recognized as the hub of the Coachella Valley with a stable economy built on agriculture, distribution of goods and services, light manufacturing, and tourism. Indio emerged as a second seat of government for Riverside County with the 1968 opening of a \$2.3 million, four-story administrative building at the junction of Highway 111 and Oasis Street.

In 1966, Indio Community Hospital was rededicated as John F. Kennedy (JFK) Memorial Hospital. The Hospital became a Tenet Healthcare facility in 1979. Today, the hospital serves the community as a 162-bed, full-service, acute-care hospital with a 24-hour emergency room, surgical services, diagnostic testing, and preventive health programs. JFK Memorial Hospital delivers more than 3,500 babies each year.



JFK Memorial Hospital Indio, CA

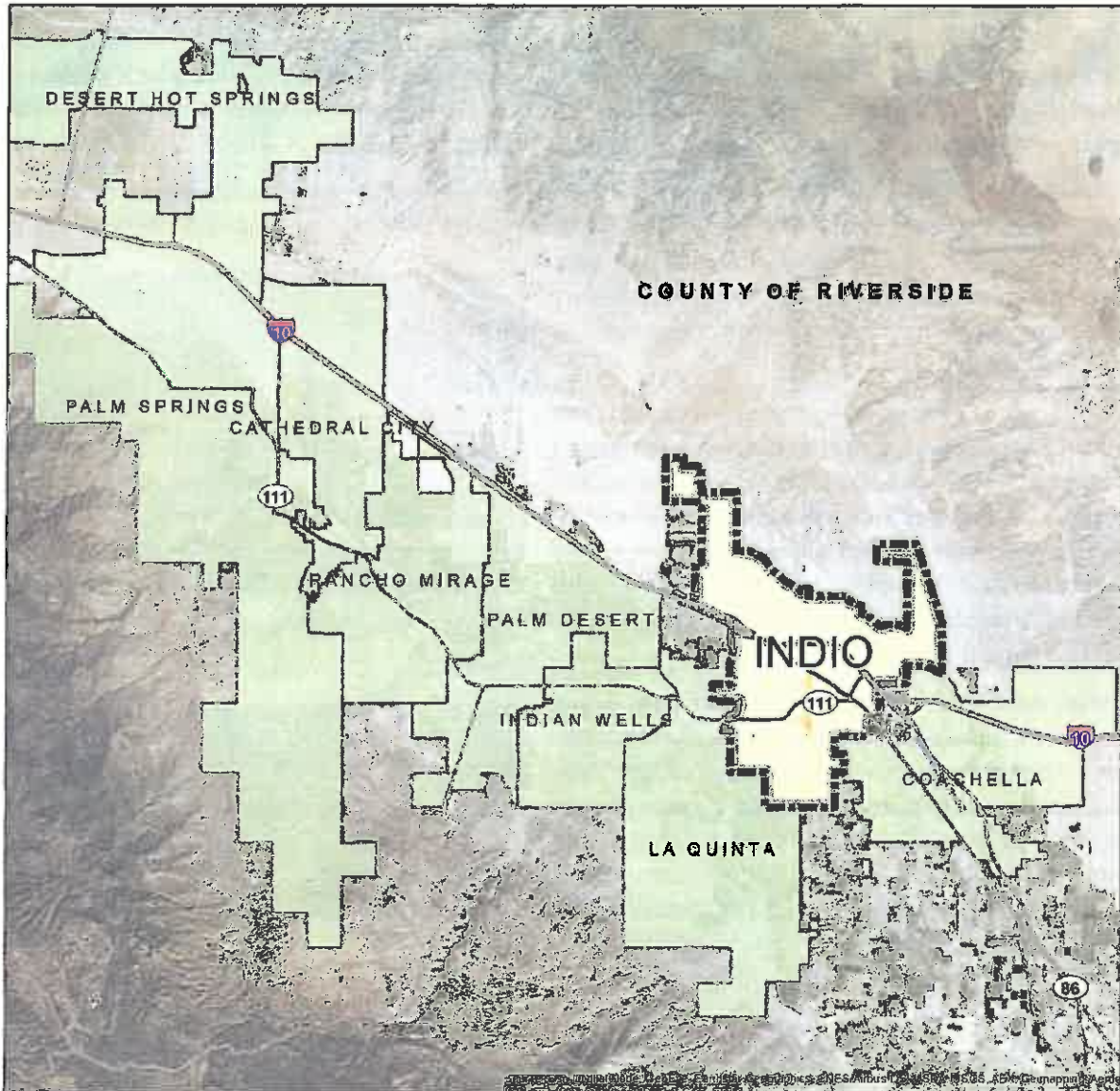
Today, Indio is known as the City of Festivals, which originated from a long history of celebrations. The Date Festival began as a celebration of the date harvest and was held in various Indio parks beginning as early as 1921. The National Date Festival was established in 1938. Present day fair attendance typically tops 270,000 people over the 10-day run.

With nearly 1.4 million visitors each year, the City is widely recognized as a cultural, music, sports, entertainment, and culinary events destination. From Native American Powwows to the Coachella Valley Music and Arts Festival, there's truly something for everyone in Indio.

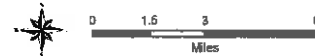


Riverside County Date Festival Indio, CA, 2017

Figure 2-1 Regional Location



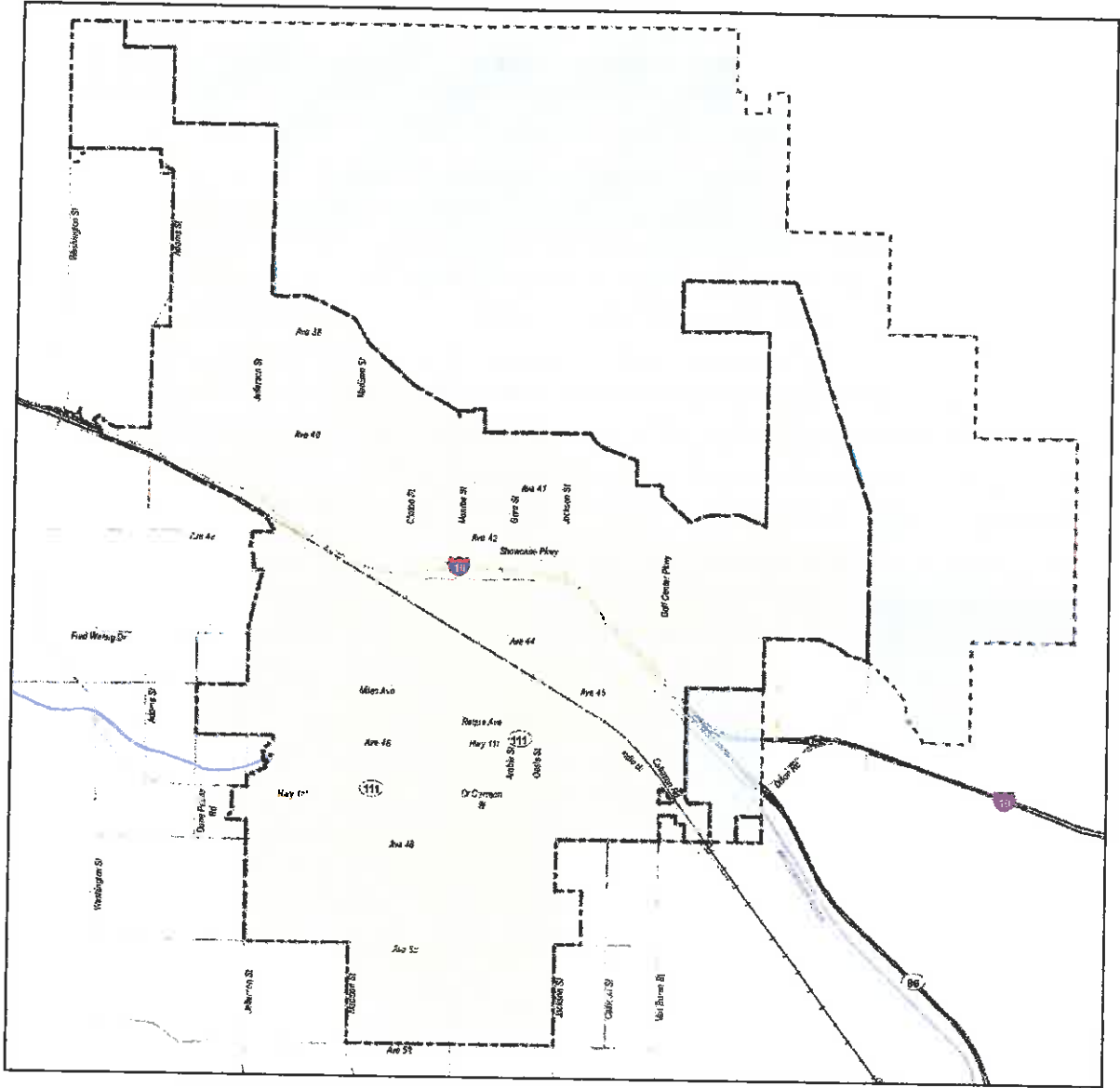
Regional Location








Source: Riverside County LAR CD, City Boundary, 2012, SOI, #107
© Riverside County 11864-04-001 (Riverside Highway) Riverside County
A0001

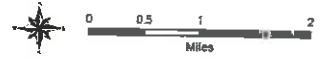
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Figure 2-2 Planning Area



Planning Area

-  City Boundary
-  Sphere of Influence
-  Roads
-  Railroads
-  Highways



Source: Riverside County/LATCO (City Boundary, 2012; SOI, 2012)
 Riverside County/TURA (Roads, Railroads, Highways) Riverside County (River)

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Population and Growth Trends

Indio is the largest and most populated City in the Coachella Valley. According to the U.S. Census, Indio had an estimated population of 88,000 in 2018. Between 2010 and 2018, it was the 10th fastest growing city in California, adding approximately 12,000 new residents as shown in Table 2-1.

Indio is expected to continue to experience significant growth in the future. According to the Southern California Association of Governments (SCAG), Indio is expected to reach a population of approximately 123,000 by 2040, an increase of approximately 39%.² In addition to its permanent residents, thousands of people reside in the City during the winter months and festivals. It should be noted that, other special events attract nearly 1.4 million visitors annually to the City.

Indio has one of the younger populations in the Coachella Valley with a median age of 35.4 as of 2016.³ Approximately 30% of Indio's residents were under age 19 in 2016, consistent with Riverside County. The City has more adults over 65 than Riverside County (approximately 15.7% compared to 13.2%).

Indio is expected to experience an influx of Millennials (typically those born between 1981 and 1997). Planning for the needs of this group is an important component of this General Plan.

Table 2-1 Indio Growth Projections

	Existing			Indio 2040 Growth Projections***		
	2010*	2018**	Incremental Growth (2018 - 2010)	2040	Incremental Growth (2040 - 2010)	Compound Annual Growth Rate
Population	76,036	87,883	11,847	119,857	43,821	1.6%
Households	23,378	26,234	2,856	38,203	14,825	1.7%
Household Size (Ave)	3.25	3.35	4.15	3.14	2.96	N/A
Employment	14,925	15,565	640	24,792	9,227	2.0%

*Existing 2010 data collected from the US Census (population, households, and household size) and Census LEHD for employment

**Existing 2018 data collected from the California Department of Finance (populations, households, household size) and Census LEHD for employment (2015)

*** Indio 2040 Projections developed by Metropolitan Research + Economics (average projection value) employment growth from 2015

² Southern California Association of Governments. 2016 Regional Transportation Plan/Sustainable Communities Strategy Draft Dataset for Indio.

³ U.S. Census. Indio city, California. (2009-2013 American Community Survey 5-Year Estimates).

What is a General Plan?

A General Plan is a city policy document required by California state law (Government Code Section 65300-65303.4) that provides a “long term, comprehensive, integrated, internally consistent and compatible statement” of goals and policies that reflect local conditions and the community vision. Within this general requirement, some aspects of the general plan are tightly prescribed, while others are left to the discretion of individual cities or counties.

Statutory Requirements

The plan serves as a basis for decisions that affect Indio’s future. The law requires that a General Plan address the following eight mandatory subject areas, or “elements:”

- + **Land Use.** The land use element identifies the location and intensity of land uses throughout the City.
- + **Circulation.** This element plans for the transportation system including roadways, transit, bicycle and pedestrian facilities; it can also address sewer, gas, water, and other infrastructure conveyance systems.
- + **Housing.** The housing element seeks to accommodate housing needs for all incomes, groups with disabilities, and the homeless.
- + **Open Space.** The open space element identifies parks and open space throughout the City.
- + **Conservation.** This element guides the use and conservation of natural resources such as soils, wildlife, water, energy, and historic resources.
- + **Noise.** This element seeks to limit the community’s exposure to excessive noise.
- + **Safety.** The safety element seeks to reduce the risk of death, injuries, property damage, and economic and social dislocation from natural and human-made hazards.
- + **Environmental Justice.** This element seeks to reduce the unique or compounded health risks of disadvantaged communities.

State law also allows for optional “elements” and for elements to be organized or combined at the City’s discretion. This general plan includes four optional elements: Community Facilities and Infrastructure, Health and Equity (inclusive of Environmental Justice), Economic Development, and Implementation.

Relationship to Other Documents

Zoning

The General Plan is primarily implemented by the Zoning Ordinance. The Zoning Ordinance specifies detailed development regulations, such as permitted uses and building standards for the zoning classifications that implement the General Plan land use designations. State law requires that the Zoning Ordinance be consistent with the General Plan.

Subdivision Map Act

The Subdivision Map Act authorizes cities and counties to regulate and control the design and development of subdivisions including, but not limited to, public improvements, street design, drainage and sanitary facilities, and lot size configuration. California Government Code Section 66473.5 states that before any city or county approves a subdivision map (including parcel maps), the public agency must determine that the proposed subdivision is consistent with the policies and objectives of the General Plan. The City of Indio's Subdivision Ordinance sets forth the requirements for the subdivision of land and implements the General Plan and Subdivision Map Act.

California Environmental Quality Act (CEQA)

A general plan is considered a project under the CEQA. CEQA requires state and local agencies to identify the potential environmental impacts of a project and avoid or mitigate those impacts to the extent feasible. A Program Environmental Impact Report (PEIR) has been prepared to identify potential environmental impacts and feasible mitigation measures for Indio's General Plan Update. To the extent possible, mitigation measures have been integrated into the General Plan goals and policies such that the General Plan becomes a self-mitigating document. As a result, there are very few impacts that would occur solely on the basis of implementation of the General Plan.

The General Plan PEIR also streamlines the review of future development projects by allowing the CEQA documents for subsequent projects to "tier" off of the General Plan PEIR. "Tiering" refers to incorporating by reference the analysis of general matters contained in a broader EIR. This allows future development projects to concentrate on the issues specific to the particular project.

Maintenance and Update of the Plan

The Indio General Plan will be implemented over the next 20 years with a time horizon of 2040. During this time, the long-range planning efforts for Indio will continue using the goals, policies, and implementation programs in this General Plan to guide growth, development, and conservation. However, the General Plan is a living document. As the City grows and changes, it will be necessary to amend maps, specific policies, and implementation actions to address changing economic and demographic conditions. In fact, State law encourages annual reviews of implementation actions and recommends that the entire General Plan be thoroughly reviewed every five years to ensure it is still consistent with the community's goals. Any part of a general plan may be amended to accommodate changing conditions.

Property owners, the Planning Commission, the City Council, or City staff may propose amendments. Proposed changes must be reviewed by the Planning Commission and the City Council at public hearings and potential environmental impacts must be evaluated in accordance with the CEQA. Community members, neighborhood groups, and local organizations are encouraged to get involved in the ongoing planning efforts of the City and to participate in the implementation of the General Plan. By maintaining active engagement in the process, Indio's residents can be part of shaping the City to make it an even more inclusive, vital, and thriving City than it is today.

Plan Organization

This General Plan is comprised of the State-mandated elements and additional optional chapters as listed below:

- + The **Land Use and Community Design Element** designates the general distribution, location, and extent of residential, commercial, industrial, public facility, open space, and other public or private land uses. It establishes design criteria to contribute to a positive character for Indio while welcoming design innovation and flexibility. It also organizes Indio into distinct subareas and establishes policy guidance that will direct the development and conservation of those areas.
- + The **Mobility Element** is closely correlated to the land use element and identifies the general location and extent of existing and proposed thoroughfares, transportation routes, terminals, airports, railroads, and other transportation facilities. It plans for a balanced, multi-modal transportation network that meets the needs of all users of streets, roads, and highways including bicyclists, children, persons with disabilities, motorists, movers of commercial goods, pedestrians, users of public transportation, and seniors.
- + The **Economic Development Element** provides a set of policies goals and objectives that are geared towards promoting sustainable, fair, and equitable economic growth in the City of Indio. The Element helps guide the City towards a more diverse local economy and improved quality of life.
- + The **Public Health and Equity Element** addresses public health considerations including access to healthy food sources, medical services, and opportunities to engage in healthy and active living, and addresses environmental justice by seeking opportunities to reduce disadvantaged communities associated and compounded health risks.
- + The **Parks and Recreation Element** details plans for Indio’s recreational open space network including parks, trails, civic plazas, community gardens and other urban agricultural activities, open space corridors that link recreation areas, and scenic resources, such as Indio Hills.
- + The **Conservation Element** addresses the preservation and conservation of natural resources including, water resources, energy resources, mineral resources, biological resources, wildlife habitats, and cultural, paleontological, and archaeological resources. It also addresses climate change.
- + The **Community Facilities and Infrastructure Element** provides goals and policies for the sustainability of the water supply and distribution system, sanitary sewer system, drainage and water quality facilities, and solid waste disposal.
- + The **Safety Element** establishes policies to protect the community from risks associated with natural and manmade hazards, such as seismic, geologic, flooding, and fire hazards as well as exposure to hazardous materials. It also addresses the potential impacts of climate change on Indio. Crime prevention and the availability of public safety services and facilities to support existing and future development are also addressed.

- + The **Noise Element** identifies current and potential future sources of noise and provides goals and strategies necessary to protect the community from excessive noise levels.
- + The **Implementation Element** houses all implementation actions for each Element within the General Plan. The Element includes a matrix of programs and physical improvements, specifying the priority, timing, and parties responsible for carrying out implementation.
- + The **Housing Element** (under separate cover) assesses current and projected housing needs and establishes policies and programs for improving housing and providing adequate housing for all income levels. The housing element is required to be updated every eight years.

How to Use This General Plan

Each element of the General Plan is organized with the following sections:

- + **Key Considerations, Strategies, and Context.** A summary of the key topics and issues facing the community. The issues serve as background for the goals and policies noting key issues, opportunities, and challenges in Indio.
- + **Goals, Policies, and Actions.** Each element contains goals, policies, and actions necessary to achieve the community's vision. They are intended to provide clear direction on how the City will implement the overall vision of this plan.

Together, the General Plan language creates a hierarchy of goals and policies that will be mandated, encouraged, or allowed by the City over the next 20 years.

Goals, Policies, and Actions

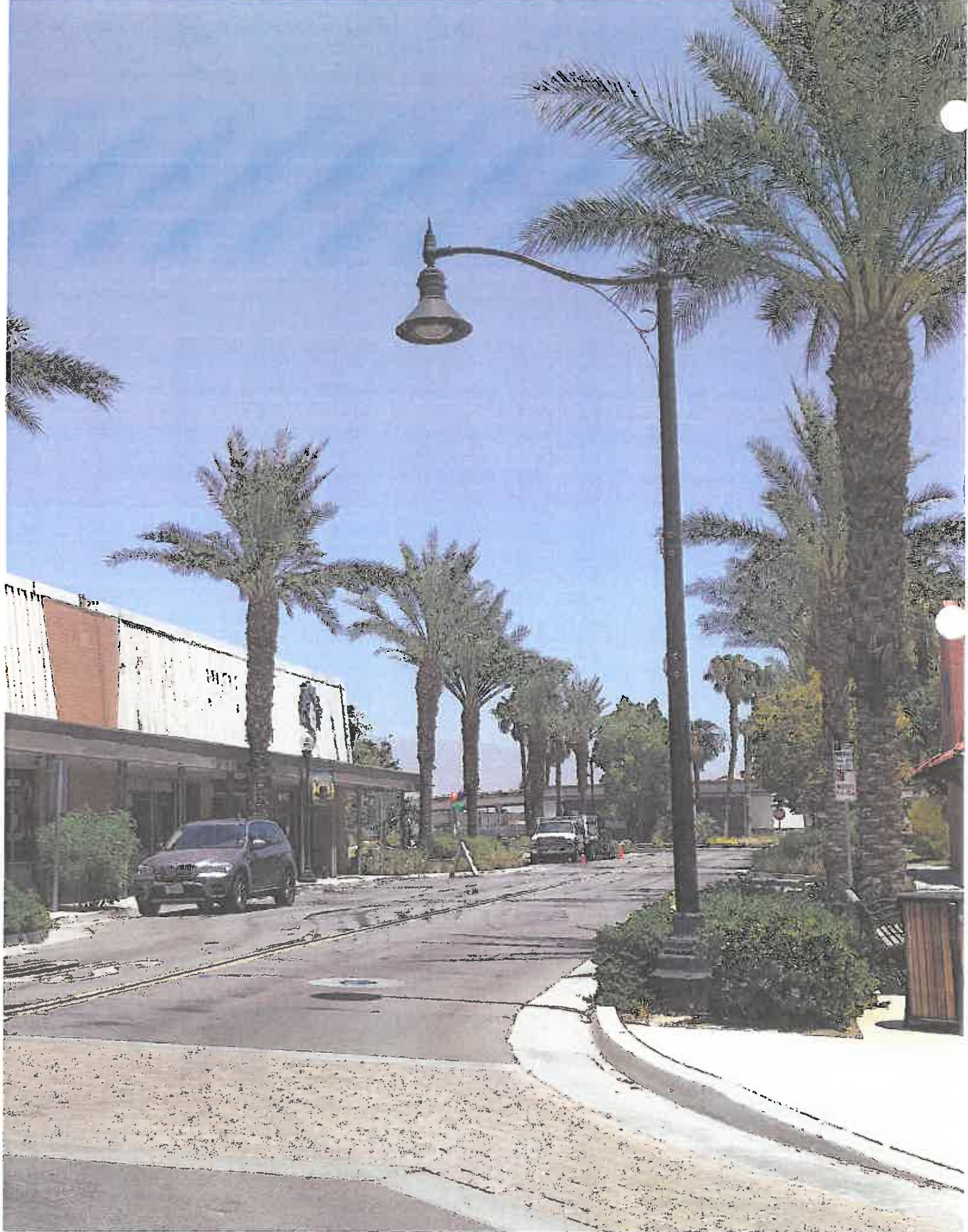
Each element of this General Plan contains goals, policies, and actions. Implementation actions are also included in the Implementation Chapter.

Goals are an overarching statement describing the overall vision and direction for the community. Goals are broad in both purpose and aim, but are designed to establish directions and outcomes. The following is an example of a goal:

- **Goal LU-2: Active Places.** Indio is a City with active and comfortable places that encourage social interaction and community gathering.

Specific position statements that support the achievement of goals and serve as guides to the City Council, Planning Commission, other City commissions and boards. City staff also use the policies when reviewing development proposals and making other decisions. Policies seek to achieve the goals by mandating, encouraging, or permitting certain actions. Certain policies are critical and must be implemented. Thus, compliance with the policy or action is mandatory. Language used to describe this intent includes will, must, require, prohibit, conduct, maintain, and implement. Other policies are strongly encouraged by the City, but total implementation may not be possible; thus compliance is not mandatory. Language used to describe this intent includes should, may, encourage, consider, explore, allow, discourage, and promote. The following are examples of policies in the Indio General Plan Land Use and Community Design Element:

- **LU-2.2 Balanced Land Uses.** Maintain a balanced land use pattern to support a broad range of housing choices, retail businesses, employment opportunities, educational and cultural institutions, entertainment spaces, and other supportive uses and within long-established Indio neighborhoods and new growth areas.
- **LU-2.4 New Gathering Spaces.** Require new developments to provide public parks, plazas, and squares that establish interesting gathering spaces in planned districts and neighborhoods.





chapter three

LAND USE AND URBAN DESIGN

This Element provides the long-term vision, goals, policies, and implementation actions for land use, development, and urban design in Indio over the next 20 to 25 years. Land use is a State-mandated element, and topics covered include the location, distribution and intensity of future land use, the form and character of future land use, enhancement of the Downtown and Midtown areas, preservation of existing neighborhood character, development of new growth areas, and mixed-use corridor revitalization.

This Element also includes an extensive treatment of community urban design. While not required by the State, it is one of the central components of this General Plan since understanding characteristics of the built environment – the location and design of our homes, stores, parks, offices and the way that we interact with these various places in the public realm – is vital to strengthening quality of life. Setting the appropriate design parameters for future change and redevelopment is critical to realizing the community’s vision.

Building on citywide goals and policies, the Element also provides a vision and key strategies for each of the City’s unique neighborhoods, districts, and centers, delineating strategies for the desired uses, character, and economic development opportunities for each area.

Key Considerations and Strategies

While a clear vision of Indio’s future is the starting point and the target for the General Plan, the City’s vision for the future will only be realized by ensuring that each new private development and each new public improvement builds towards that future. The following strategies shall guide public and private actions and investments to achieve the vision, and are reflected in the General Plan Place Types, neighborhood subarea descriptions, and policy framework.

Infill First

- + **Make it Happen:** Focus on target subareas, such as Downtown and Midtown, reinvesting in and connecting existing neighborhoods, while limiting the expansion of the City’s development footprint.
- + **Let it Happen:** Allow the 42nd Street corridor to happen, ensuring the result is a connected, complete place with housing and commercial uses organized into a walkable, transit-supportive pattern of neighborhoods and centers.
- + **Let it Wait:** Delay annexation and development in the Northern and Eastern Sphere of Influence subareas at least until existing large neighborhood infill areas are substantially built. Allowing development in these subareas will reduce market support for infill neighborhoods, result in adverse environmental and public impacts, and strain limited financial resources for construction and maintenance of infrastructure and provision of municipal services to a disconnected patchwork development pattern.

Plan and Build Places

- + **Active:** Integrate a range of housing types, amenities, and uses within compact, walkable neighborhoods. New uses and activities will be co-located in centers to encourage social gathering and interaction and to reduce vehicle miles traveled.
- + **Comfortable:** Reinforce human comfort in new and existing neighborhoods by making and sustaining safe, clean, and accessible places for all of Indio’s residents.
- + **Accessible:** Ensure convenient transportation connections, including roadways, sidewalks, trails, and efficient transit that provide safe and comfortable access by all travel modes.

Connect at All Scales

- + **Buildings to the Street:** Orient housing, retail shops, offices, and civic buildings to their adjacent streets so that streets are safe, pleasant, and provide useful linkages and spaces for daily trips by all modes.
- + **Neighborhoods to the Amenities:** Connect new and existing neighborhoods to adjoining neighborhoods, parks, schools, and retail shops through high-quality streets and pathways designed to facilitate walking and biking.
- + **Neighborhoods to Centers and the Rest of the City:** Provide route choices for pedestrians, bicyclists, and motorists to Downtown, Midtown, and other key centers, avoiding overloading major streets with traffic and providing mobility choice to all residents. Mobility choice is particularly critical for the very old, very young, and very poor who have reduced access to automobiles.

Generate Habitable, Human-Scaled Public Realm

- + **Front Yards:** Focus on front yards, which along with the streets they face form the “outdoor rooms” of the City in which residents live, shop, and play.
- + **Parks and Open Spaces:** Form an integrated open space network that unifies the City, weaving together neighborhoods, centers and employment districts. Design neighborhood parks and other special public spaces to enhance the value and pride of the neighborhoods around them.
- + **Safe:** Orient buildings towards public spaces to provide “eyes on the street” to create welcoming and safe places for Indio residents.

Focus on Specific Areas

- + **Focus as Strategy:** From the original small town around the Downtown, Indio has grown rapidly in patchwork pattern over the past 50 years. Realizing the new General Plan’s vision for better connecting those pieces and infilling the gaps will require many decades of work. Only through focusing finite resources of staff time and resources and City and developer capital on high-priority areas will residents enjoy the benefits of significant, tangible improvement within the next 10 to 20 years.
- + **Downtown and Midtown:** Focus on Indio’s historic Downtown and surrounding neighborhoods, major opportunity sites along Highway 111, and the County Fairgrounds. Collectively, these areas represent an enormous opportunity to realize many of the goals of this Plan, and to anchor civic and economic activity for the benefit of all residents.
- + **Entertainment District:** Build on the success of Indio’s festivals and special events, which draw the world’s attention and investment, to support collateral development and improvements in Indio is a top priority.
- + **42nd Street corridor:** Establish a pattern of connected neighborhoods, centers, and employment districts along both sides of Avenue 42 to leverage the economic and fiscal value of that area for future generations.



Miles Avenue Museum



Downtown Indio

Context

Land Use

Table 3-1 describe the amount and current distribution of land uses in the City of Indio. Approximately one-quarter of the total City developed land area is occupied by residential uses, the majority of which is single-family housing. The next largest land use category is transportation, communication, and utility at one-quarter of the total developed land area of the City. This is followed by agricultural uses, which equates to 17 percent of the land area.



Typical residential neighborhood in Indio

Public lands account for approximately 20 percent of the total developed land area of the City. This includes institutional and public facilities, and parks, open spaces, and recreation. Retail, office, industrial and lodging uses account for 12 percent of the total developed land area of the City.

Table 3-1: Existing Land Uses

Land Use	Acreage	Percent of Planning Area	Percent of Developed Area
Rural Residential	472	1%	2%
Single-Family Residential	3,913	10%	21%
Multi-Family Residential	220	< 1%	1%
Mobile Home Parks	450	1%	2%
Commercial	1,029	3%	5%
Office	204	< 1%	1%
Industrial	958	3%	5%
Public Facilities	615	2%	3%
Transportation/Communication/Utilities	4,954	13%	26%
Open Space/Recreation/Horse Facilities	1,926	5%	10%
Agriculture	3,203	8%	17%
Water	98	< 1%	1%
Vacant	20,145	52%	N/A
Under Construction	887	2%	5%
Total	39,074	100%	100%

Urban Form and Pattern

Indio has an urban pattern that includes established residential areas and commercial corridors in the historic part of the City and suburban housing and commercial uses along the edge of the City.

The City's core residential neighborhoods are built at moderate densities with consistent setbacks in small blocks with a well-connected street networks. New subdivisions have a more suburban building layout, with larger yards and less regular building placement within large blocks with more fragmented street network. Most of the City's commercial districts, such as Highway 111, Monroe and Jackson Street, are developed in a strip commercial development style; however, Miles Avenue has a more traditional main street commercial-style. There are large building footprints in the City scattered throughout, and the hotels/big box retail around the intersection of Jackson Street and I-10, among others.

Different patterns also emerge at the block level. Small, walkable blocks less than 2 acres in size dominate the older, central areas of the historic City. Moderate blocks of 5 to 25 acres are most common in the suburban expansions along the edge of the city core. Superblocks larger than 50 acres are found in the northern part of the City. There are numerous, very large, undeveloped areas scattered throughout the City. These large undeveloped areas can be a potential asset, as it allows coordinated development adjacent to existing development with a unified vision.

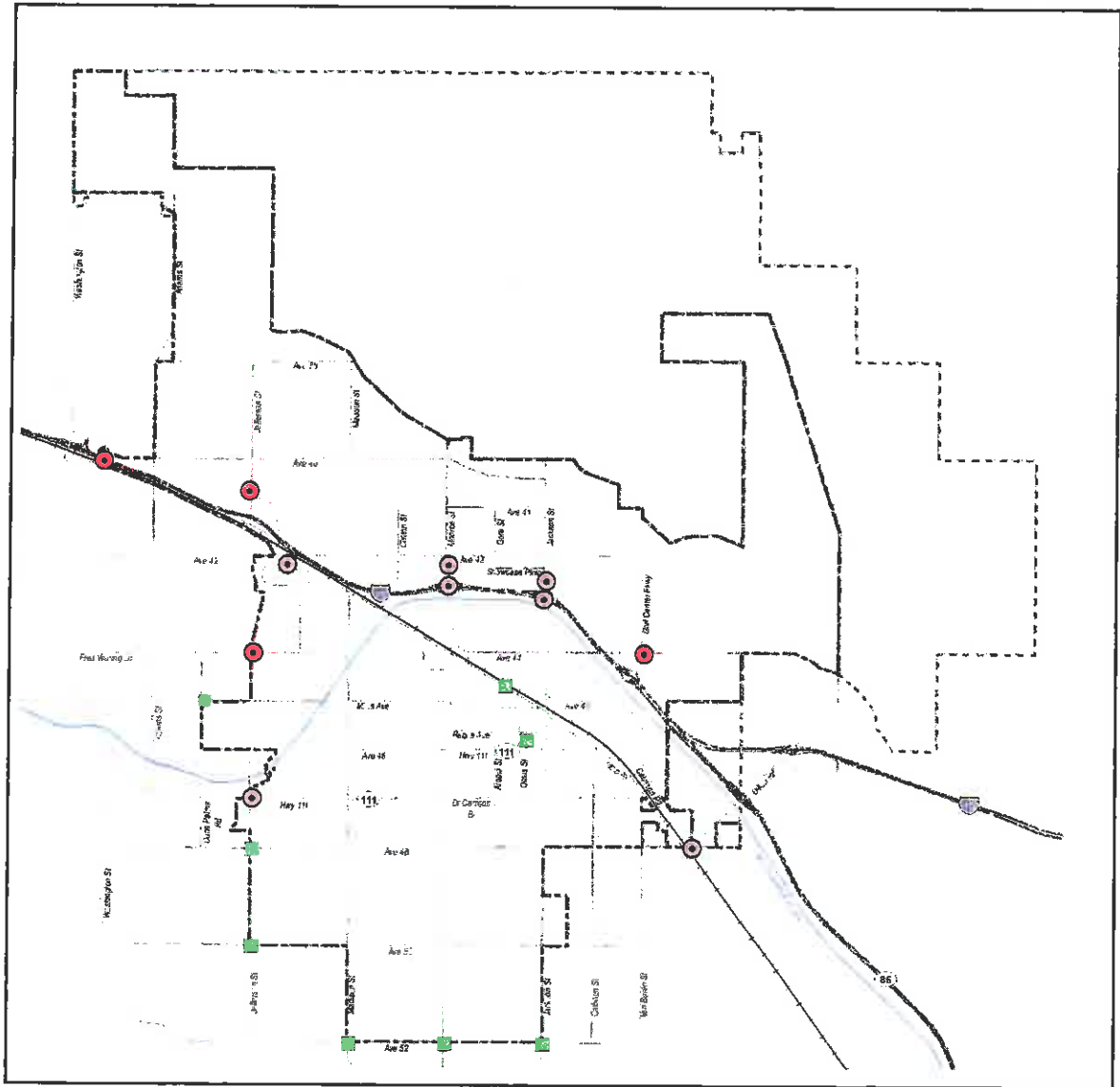
Gateways

Gateways are identified entrance points into the City that are (or could be) enhanced with unique design features, such as monuments, gateway markers, art, or signage. Currently there are nine primary gateways into the City and eight secondary gateways. The majority of gateways, whether primary or secondary, do not include gateway elements. These gateways are mapped on Figure 3.2.








Primary Gateways

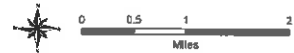
- Jackson Street, north of I-10: This primary gateway is largely undeveloped. Currently there is no signage at this intersection.
- Jefferson Street, south of I-10: This primary gateway is largely undeveloped. Currently there is no signage at this intersection.
- Jefferson Street at Fred Warring Drive: This western gateway is a mixed commercial, residential, and undeveloped intersection. There are no gateway elements provided at the intersection.
- Jefferson Street at Highway 111: This western gateway is under construction. It is largely a commercial intersection.
- Avenue 48 at Indio Boulevard: This eastern gateway is largely a commercial and undeveloped intersection. There are no gateway elements provided at the intersection.
- Avenue 44 and Golf Center Parkway: This northeast gateway is mostly undeveloped. There are no gateway elements provided at the intersection.
- Avenue 42 and Jackson Street: This is largely a commercial intersection with no gateway elements. There are no gateway elements provided at the intersection.
- Avenue 42 and Monroe Street: This northeast gateway is mostly undeveloped. There are no gateway elements provided at the intersection.

Figure 3-2: Gateways



Gateways

-  City Boundary
-  Sphere of Influence
-  Roads
-  Railroads
-  Highways
-  Primary Gateway
-  Secondary Gateway

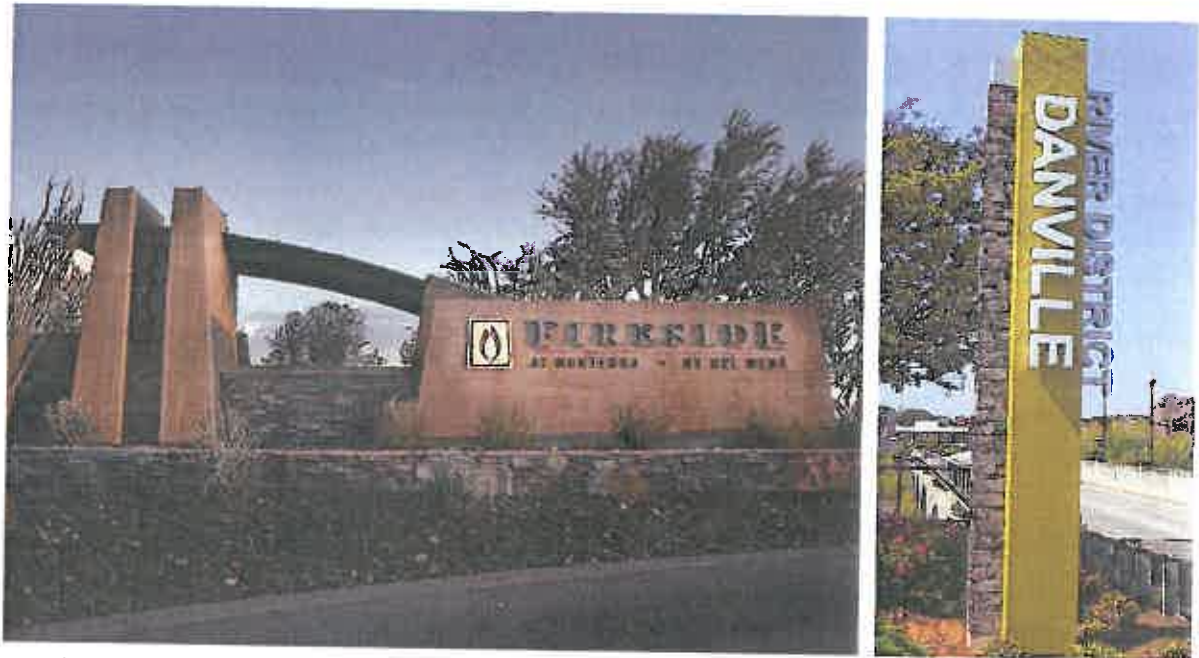


(Source: Riverside County AFD01 City Boundary 2012, 801 2012)
 Riverside County IEMA/Roads, Railroads, Highways Riverside County (State)

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Secondary Gateways

- Oasis Street and Highway 111. This central gateway is a commercial intersection. This is a key entrance to Downtown Indio. There are no gateway elements provided at the intersection.
- Arabia Street and Indio Boulevard. This is an entrance into Downtown Indio. The gateway has a mix of commercial and park uses. There are no gateway elements provided at the intersection.
- Dune Palms Drive at Miles Avenue: This secondary, western gateway is largely a residential intersection. There are no gateway elements provided at the intersection.
- Jefferson Street at Avenue 48: This western gateway is largely a residential intersection. There are no gateway elements provided at the intersection.
- Jefferson Street at Avenue 50: This western gateway is a mixed commercial, residential, and undeveloped intersection. There are no gateway elements provided at the intersection.
- Madison Street at Avenue 52. This gateway is a mixed residential and undeveloped intersection. There are no gateway elements provided at the intersection.
- Monroe Street at Avenue 52. This gateway is largely undeveloped. Currently there is no signage at this intersection.
- Jackson Street at Avenue 52. This gateway is largely a commercial and undeveloped intersection. There are no gateway elements provided at the intersection.



Sample Gateway elements

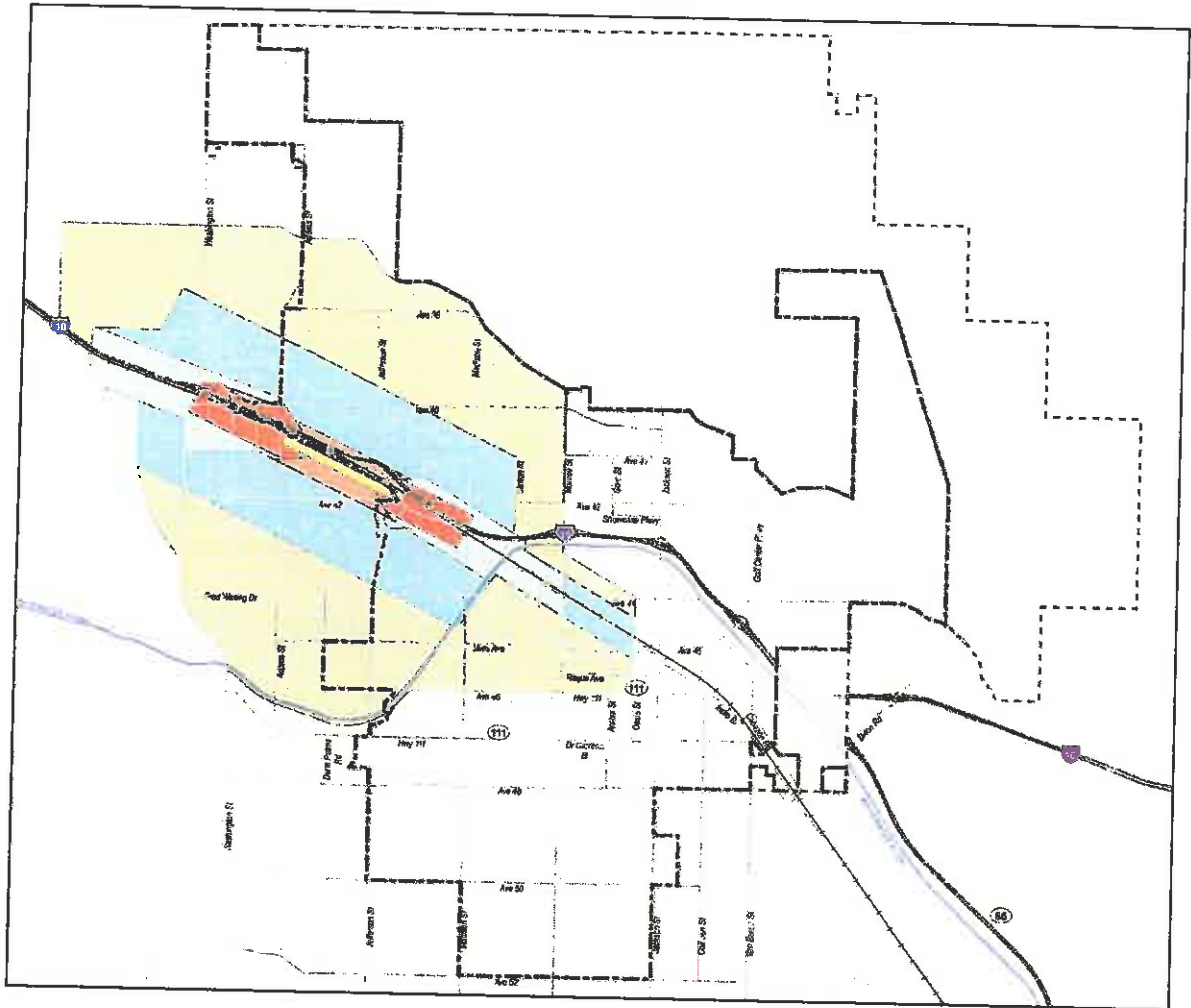
Bermuda Dunes Airport

The Bermuda Dunes Airport is a privately-owned, public-use general aviation airport located along the central, western edge of Indio. It is an operational facility currently used by businesses, charters, medical, and other private and recreational entities. The airport services approximately 11,500 flights each year, with seasonal peak periods. As a consequence of the airport, certain restrictions apply to new projects within its adjacency as identified in the adopted Riverside County Airport Land Use Capability Plan (CLUP). Specifically, there are limits on the maximum structure height, use restrictions, and mitigation and other treatments for new development built within the noise contours identified in the CLUP. Figure 3-3 illustrates the compatibility zones and Figure 11-2 shows the noise contours.



Bermuda Dunes Airport

Figure 3-3: Bermuda Dunes Airport Capability Map

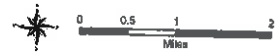


Planning Area

- Roads
- Railroads
- Highways
- City Boundary
- Sphere of Influence

Airport Compatibility Zones

- A
- B1
- B2
- C
- D
- E



Source: Riverside County/LAFCD (City Boundary 2012; 801,2012); Riverside County/TJMA (Roads/Highways) Riverside County (2007)

The information on this map was derived from various digital databases, SOURCE above. Care was taken in the creation of this map but it is provided "as is". TDC cannot accept any responsibility for any errors, omissions, or positional accuracy, and therefore, there are no warranties which accompany this product. Users are cautioned to fully verify information on this product before making any decisions.

Areas of Change

The General Plan Place Types Map (Figure 3-5) identifies the future intended use of land on every parcel in the City. Equally important is to identify the locations where growth and development is expected (or desired) to occur and locations that should be protected from future development. These characterizations allow the public and decision-makers to have a common understanding of where change will occur, the vision for change in each area, and the overall level of intensity. Equally important, this map signals to the community that certain areas are to be protected from new development and land use changes.

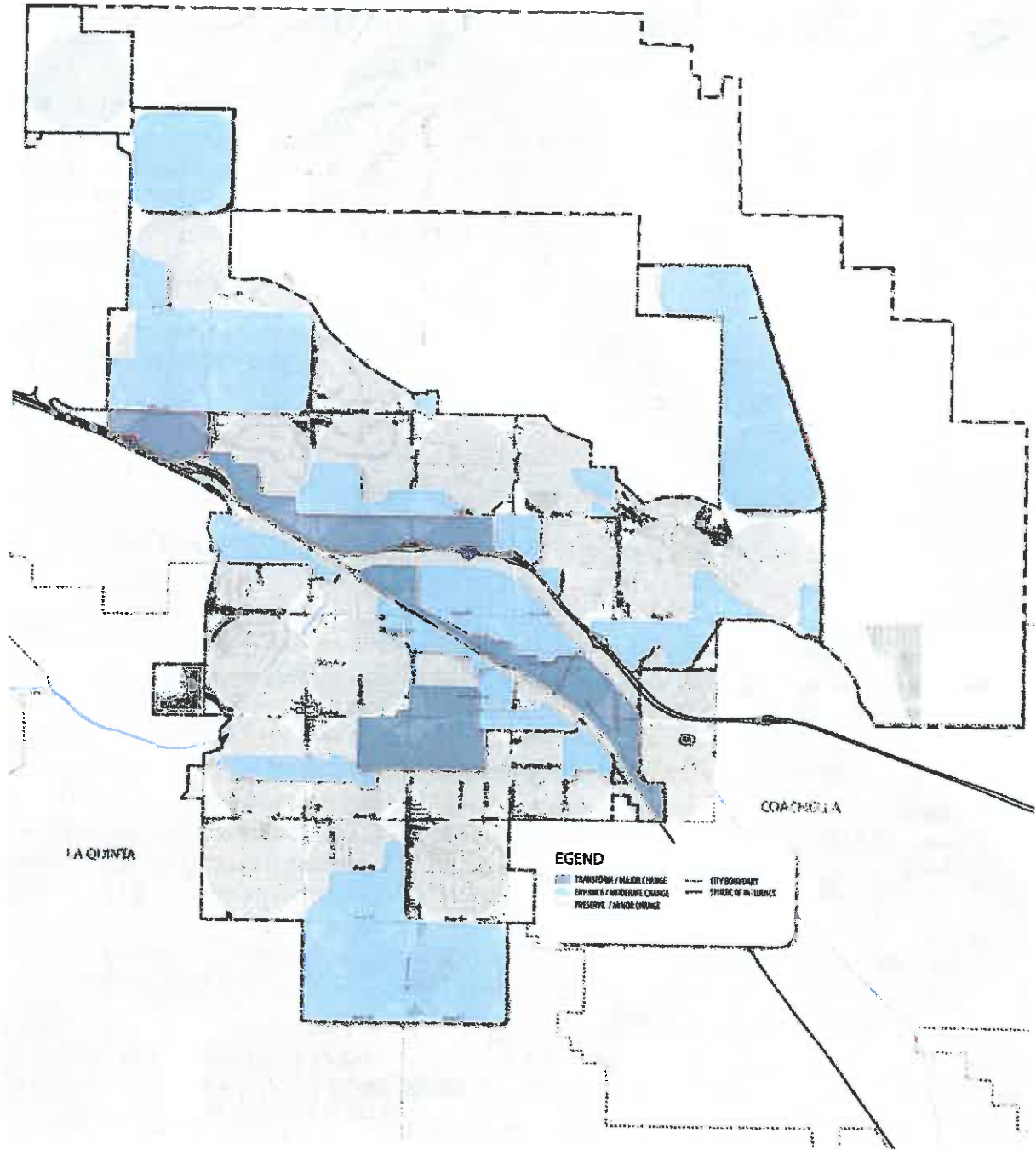
Figure 3-4 indicates the location and degree of change for key area within the City. These change areas were defined by consolidating existing plans for each neighborhood, analyzing existing conditions, opportunities, and trends, and facilitating discussions with stakeholders and the community. These also indicate the priority level for each area of change. Definitions of the three categories are as follows:

- + **Preserve, Minor Change:** Areas of the City where the general character of the area will remain the same but improvement is desired in limited ways. This includes but is not limited to improvements to the public realm, the addition of a second story to a single-story home or the addition of community facilities or neighborhood-serving retail uses that are desired but currently absent. The majority of the residential areas and some of the commercial areas are classified as preserve and enhance.
- + **Enhance, Moderate Change:** Areas of the City where change is desired over the time horizon of the General Plan and where change will happen gradually over the entire horizon of the General Plan and beyond. These areas are expected to see moderate development over time and the area may, after 15 to 20 years, look very different than it does today.
- + **Transformation, Major Change:** Areas where the community wants to actively facilitate significant change in the short to middle term. These areas may look very different in a short period of time and these are areas where the City of Indio may want to prioritize staff and financial resources or actively encourage new private development.



Area of transformation; Indio Fashion Mall in the Midtown Area

Figure 3-4 Degree of Change



General Plan Designations

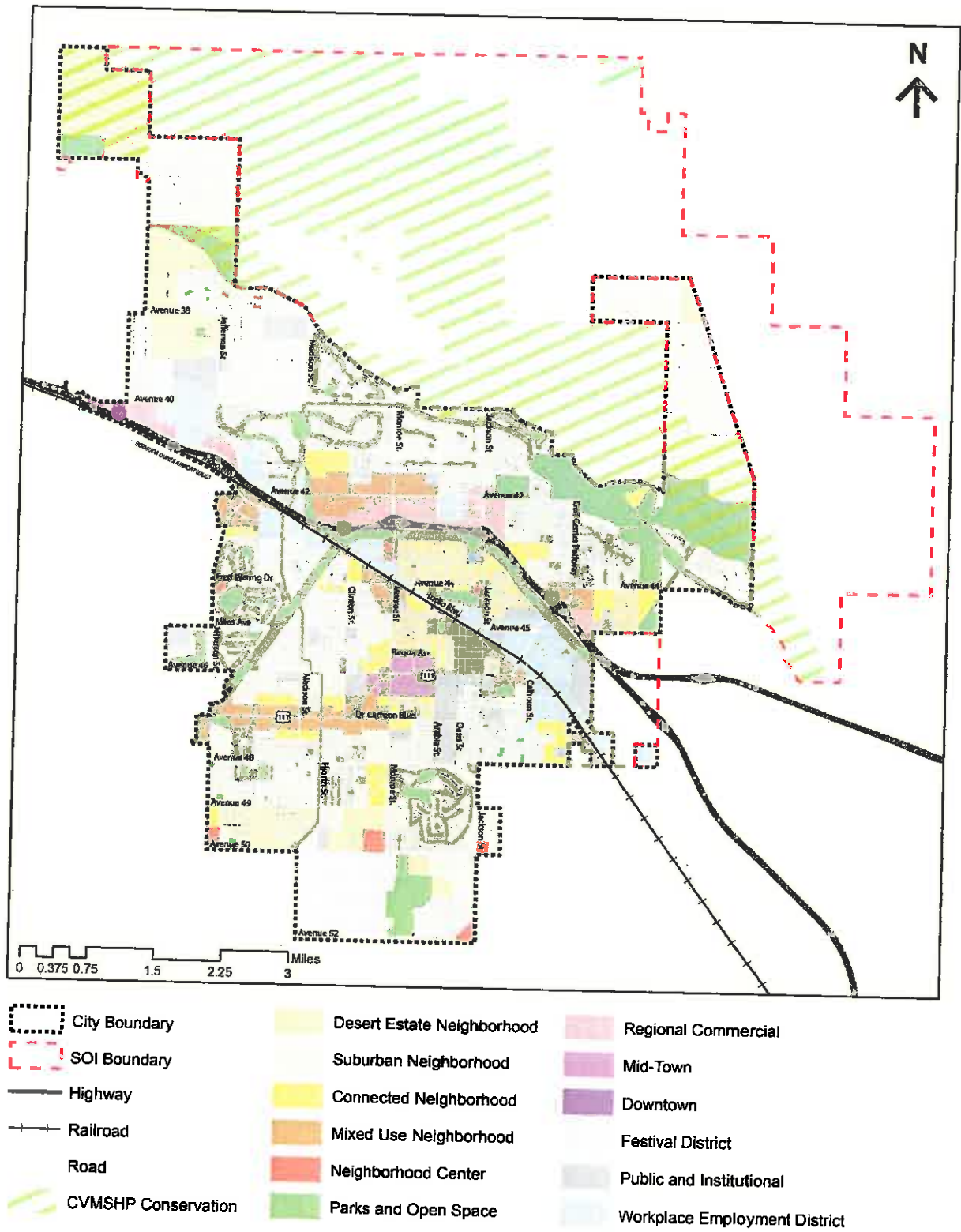
The General Plan sets forth land use designations called place types that indicate the purpose and intended use for each parcel within the City. These place types are developed to provide clear, yet flexible, structure that adapts to changing economic conditions and community vision.

The land use place types are organized into three broad categories: neighborhoods, centers, and district. Each place type may contain a mix of uses, including residential, commercial, industrial, and public uses, at a variety of scales and intensities. Given Indio's place-making goals and the community's interest in shaping the form and character of their City, each place type provides direction on use, intensity/density, form, and character. Citywide goals and policies are organized by broad categories of place types (neighborhoods, centers, and districts).

- + **Neighborhoods** are the basic building block of all great cities and this General Plan identifies a range of neighborhood types for Indio. Neighborhoods include a balanced mix of activity that includes a variety of dwellings, small shops and workplaces, civic buildings, and parks. The vision of the Plan is to create complete, compact, and connected neighborhoods that provide a high quality of life for residents. Future Indio neighborhoods should mix a variety of residential types, parks, schools, and neighborhood centers within a walkable network of streets. The public spaces should be safe, attractive, accessible, and comfortable places to live and play. The General Plan includes several types of residential neighborhoods including Desert Estate, Suburban Neighborhood, Mixed Use Neighborhood, and Connected Neighborhood. Resorts may be incorporated into any neighborhood type.
- + **Centers** are concentrations of neighborhood-serving commercial uses, medium- and higher-density housing, and employment uses in a walkable urban environment. Centers bring goods, services, amenities, arts and culture, and civic activities within a short distance of residents. Centers are scaled for compatibility with nearby neighborhoods. They can be larger or smaller, more or less intense depending on the location in the City and the adjacent neighborhoods. Several of types of centers are defined, including Downtown, Midtown, and Neighborhood Centers.
- + **Districts** are areas with larger concentrations of non-residential uses, typically dominated by a single activity. They include regional shopping centers, auto dealership centers and other auto service uses, larger manufacturing uses (excluding small office and maker / Research & Development uses). Districts play an important role in Indio since they provide jobs and economic development opportunities. By their intensity and single-use focus, districts are more automobile-oriented than neighborhoods and centers. Districts can abut neighborhoods but great care must be taken in designing the connections and transitions between them. Several types of districts are defined, including the Work Place and Employment, Festival, and Regional Commercial.

The proposed Indio place types differ from traditional land use designations in that each specifies additional guidance related to development form and character. These designations include allowable uses and building density but also address building placement on a lot, parking location and access, building frontage, and streetscape design. This refinement is consistent with the community's desire to provide more specific guidance on the intended future character of the City, and allows the City to establish zoning designations that similarly communicate form and character.

Figure 3-5: General Plan Place Types



Understanding Density

State law requires that General Plan land use designations provide a measurement of the maximum development intensity allowed within each designation. The three generally accepted metrics are dwelling units per acre, floor area ration, and persons per square mile. It is important to note that density is only loosely related to urban form and character.

DU/AC – Dwelling Units per Acre

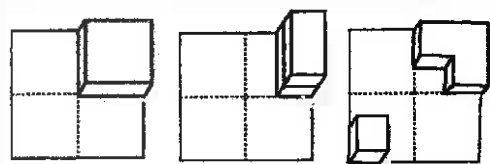
The term density is used for residential uses and refers to the population and development capacity of residential land. Density within the General Plan is described in terms of dwelling units per gross acre of land (du/ac), including existing and proposed streets and rights-of-way.

It should be noted that du/ac is not always a useful predictor of neighborhood design character or population density. While in areas of single-family detached homes, du/ac does establish how many homes—and presumably how many families—will be present in each acre, in areas of mixed-use or multi-family development it is much less helpful. For example, 2 or 3-story

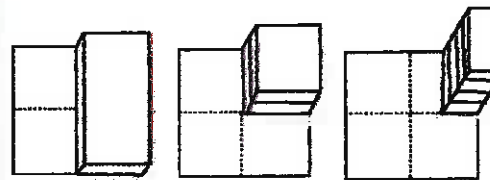
FAR – Floor Area Ratio

Development intensity, which applies to nonresidential and mixed uses, refers to the extent of development on a parcel of land or lot. Floor area ratio is used in the General Plan as a measure of non-residential or mixed-use development intensity.

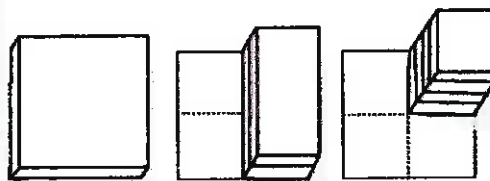
Floor area ratio (FAR) expresses the intensity of use on the lot (see Figure to the right). The FAR represents the ratio between the total gross floor area of all buildings on a lot and the total land area of that lot. For example, a 20,000 square foot building on a 40,000 square foot lot yields a FAR of 0.5. A 0.5 FAR describes a single-story building that covers half of the lot, a two-story building covering approximately one-quarter of the lot, or a four-story building covering one-eighth of the lot. This makes FAR a useful tool for measuring environmental impacts but not a useful tool for regulating urban form or neighborhood design character.



0.25 FAR



0.5 FAR



1.0 FAR

$$\text{Floor Area Ratio (FAR)} = \frac{\text{Gross Building Area}}{\text{Lot Area}}$$

Persons/sq. mi – Inhabitants per Square Mile

A simplistic estimate of the number of total inhabitants in a square mile of a given land use designation, derived from known averages of building type occupancies.

Table 3-2: General Plan Place Types

Major Classes	Place Type and Intent	Max. Gross Density/FAR*	Zoning Correlation
NEIGHBORHOODS	Desert Estates (DE). Conserves natural features while providing the lowest intensity residential neighborhood development.	DU/AC: Up to 1	EE, CEIR, CET, CE
	Suburban Neighborhoods (SN). Provides low-intensity neighborhood development for single-family, detached homes.	Up to 8 DU/AC	EE, CEIR, CET, CE, RL, RM
	Connected Neighborhood (CN). Provides a broad range of housing choices within a walkable neighborhood setting within a short distance of goods and services. Housing types include single-family detached and attached dwellings of various sizes, courtyard apartments and condos, and medium-sized multifamily buildings designed for neighborhood compatibility.	DU/AC: Up to 20 FAR: up to 0.25	RLCI, RM, CE, RM, RH, P
	Mixed Use Neighborhood (MUN). Provides moderate- to higher-intensity neighborhood development that features a variety of multifamily housing choices and commercial uses along major streets.	DU/AC: Up to 40 FAR: up to 0.5	RH, NC, CC, P
CENTERS	Downtown (DT). Supports lively, thriving Downtown area by accommodating multi-story, mixed-use buildings at higher intensities. Provides for vertical and horizontal mixed-use development.	DU/AC: Up to 60 FAR: Up to 2.0	DC, P, RH
	Midtown (MT). Provides of an active, mixed-use center in Midtown by allowing multi-story, mixed-use buildings at higher intensities, community gathering spaces, and land uses to support residents and visitors. Provides for vertical and horizontal mixed-use development.	DU/AC: Up to 40 FAR: Up to 1.0	NC, CC, RH, P
	Neighborhood Center (NC). Provides for areas with a variety of neighborhood-serving retail and commercial uses, housing of various types, other neighborhood-serving amenities, and community-gathering spaces for residents.	DU/AC: 10 to 20 FAR: Up to 0.5	NC, CC, RH, P
DISTRICTS	Workplace and Employment District (WEP). Provides an area for a wide variety of employment-generating activity, including office, industrial and light manufacturing, research and development, and supportive commercial.	Up 1.0 FAR	CO, BP, IP, M, NC
	Festival District (FD). Reserves area for outdoor festivals, events, sporting activities and related activities.	Up to 0.5 FAR	NC, MMFOZ
	Regional Commercial (RC). Provides large-format retail development along with associated restaurant and commercial service activity.	FAR: 0.35 to 1.0	RC, CC
PUBLIC AND INSTITUTIONAL	Public and Institutional (PI). Reserves areas for public, educational, and institutional uses.	Up to 0.5 FAR	P
	Parks and Open Space (OS). Provides public and private space for recreational uses, including open space, and golf course greens.	Up to 0.01 FAR	P, OS
	Resource Management and Open Space Overlay (R-OS). Provides an overlay for areas consistent with the Coachella Valley Multi-Species Habitat Management Plan, passive recreation, trails/paths, restoration, ecotourism, and environmental educational activities.	Consistent with CVMSHMP	P, OS, RR

*For larger development projects, portions of the project area may exceed the density as long as the total project density does not exceed the allowable density or FAR for the placetype.

NEIGHBORHOOD

Desert Estates

Conserves natural features while providing the lowest intensity and amount of residential neighborhood development.

Allowed Land Uses

- + Primary: Single-family residential, agriculture, parks and recreation
- + Secondary: Hospitality, recreational

Allowed Development Intensities

- + DU/AC: Up to 1.0
- + Commercial FAR: N/A

Streetscape

Streetscapes are natural, with primarily curbless and gutterless shoulders defined by informal tree arrangements, natural street edges, and drainage swales or ditches. Street alignments are relatively informal, typically following the natural topography to minimize grading disturbance of the natural environment. The typical vehicular design speed is 25 mph.



Connectivity

Block sizes are typically large, up to 2,000 feet in perimeter. Some block edges may be defined by trails or multi-use paths. Some dead end roads may be warranted to preserve existing topography and/or natural environment. Approximately 50 intersections per square mile.

Open Space

Open space is provided primarily through the preservation and integration of natural features and open space into the neighborhood design. Primary General Plan park types may include nature / open space, greenways, and trails.

Building Form and Character

In general, limited portions of sites are developed with groupings of single-family houses organized around natural areas. Buildings are set back significantly from the natural street edge to provide very-large front yards with porches or terraces. Structures are clustered together to leave most of the natural area undisturbed. Buildings are generally limited to one- and two-stories in height.

NEIGHBORHOOD

Suburban Neighborhood

Provides low intensity neighborhood development that features a limited variety of housing choices. Uses are primarily single-family houses with small low intensity multi-family dwelling groupings organized along walkable streetscapes with commercial/retail activity nearby.

Allowed Land Uses

- + Primary: Single-family residential, multi-family residential, parks and recreation
- + Secondary: Public facilities, resort

Allowed Development Intensities

- + DU/AC: Up to 8
- + Commercial FAR: N/A

Streetscape

Streets are landscaped with formal or informal street tree patterns and sidewalks on at least one side. The typical vehicular design speed is 25 mph.

Connectivity

Blocks may be large, up to ½ mile in perimeter. They are defined by streets, but blocks should provide pedestrian paseos or paths reducing the effective pedestrian perimeter to no more than 2,000 feet. Intersection density should be no less than 200 per square mile.

Open Space

Much of the neighborhood open space is provided by the generous streetscapes and landscaped front yards. Suburban neighborhoods also have neighborhood parks, linear parks, greenways, trails, and other park types. Landscape patterns may be naturalistic or formal.

Building Form and Character

Buildings are set back from the street to provide moderate to large front yards. Buildings are primarily one- and two-story single-family houses. Single-family attached and small multifamily housing – with a scale and character compatible with nearby single-family homes – may be integrated with single-family detached housing.



NEIGHBORHOOD

Connected Neighborhood

Provides intensity range of housing choices within a walkable neighborhood setting within a short distance of goods and services that meet daily needs.

Allowed Land Uses

- ✦ Primary: Range of single-family and multi-family residential uses, including duplex, triplex, quadplex, rowhouses, townhouses, courtyard multifamily buildings, and small scale multifamily buildings, parks and recreation
- ✦ Secondary: Retail, care, public facilities, guest houses, places of worship, schools, family day care homes, public facilities, and others which are determined to be compatible with and oriented toward serving the needs of neighborhoods may also be allowed. Secondary uses should be focused along mixed-use corridors or at the edges of centers.



Allowed Development Intensities

- ✦ DU/AC: Up to 20
- ✦ Commercial FAR: 0.25

Streetscape

Streetscapes have steady rows of street trees planted in parkway strips and generous sidewalks. The typical vehicular design speed is 25 mph.

Connectivity

Block perimeters should not exceed 1,600 feet, but blocks up to 2,200 feet may be allowed if a pedestrian paseo or greenway is provided near the midpoint. Intersection density should be at least 400 per square mile.

Open Space

Open space consists of the generous streetscapes and landscaped front yards and a variety of park types. Connected neighborhoods may have neighborhood parks, mini parks, greens, linear parks, greenways, trails, and other park types.

Building Form and Character

Buildings are set back to provide moderately sized front yards, facing the street with emphasis on porches, terraces, dooryards and/or fenced front yards. Buildings include a variety of housing types up to three-stories, with attached single-family and small-scale multi-family buildings massed and scaled for compatibility with adjacent homes. Small-scale non-residential uses may be incorporated in the form of live-work units and corner shops, primarily along mixed-used corridors or at the edges of centers.

NEIGHBORHOOD

Mixed Use Neighborhood

Provides moderate- to higher-intensity neighborhood development that features a variety of multifamily housing choices and limited neighborhood-serving commercial uses in a walkable environment. Allows commercial development along major streets and adjacent to a neighborhood center.

Allowed Land Uses

- ✦ Primary: Range of single-family and multi-family residential uses, parks and recreation, mixed use (retail/office, retail/residential, office/residential, and neighborhood-serving retail, general commercial, services, entertainment, live/work, and office
- ✦ Secondary: Public facilities, guest houses, places of worship, schools, family day care homes, public facilities, and others which are determined to be compatible with and oriented toward serving the needs of neighborhoods may also be allowed. Secondary uses should be focused along mixed-use corridors or at the edges of centers.



Allowed Development Intensities

- ✦ DU/AC: up to 40
- ✦ Commercial FAR: 0.5

Streetscape

Streetscapes have an urban character with formal street tree patterns and are highly interconnected. The typical vehicular design speed is 25 mph.

Connectivity

Block perimeters should not exceed 1,600 feet, which are generally deeper and shorter than other Connected Neighborhood types to accommodate deeper buildings and/or more parking. Blocks up to 2,200 feet may be allowed if a pedestrian paseo or greenway is provided near the midpoint. Intersection density should be at least 400 per square mile.

Open Space

Open space consists of the generous streetscapes and landscaped front yards and a variety of park types, particularly plazas and greens adjacent to mixed-use areas. Connected neighborhoods may have neighborhood parks, mini parks, greens, linear parks, greenways, trails, and other park types.

Building Form and Character

Buildings are set back from the sidewalk to provide moderately-sized front yards with porches and terraces except in mixed-use areas. In mixed-use areas, buildings are set near or at the back of the sidewalk to support outdoor dining and easy view of shopfronts. Buildings may include a variety of housing types up to three-stories, with mixed-use buildings up to three-stories focused at key intersections, along main corridors, and/or adjacent to public open spaces.

CENTER

Downtown

Supports lively, thriving Downtown area by accommodating multi-story, mixed-use buildings at higher densities intensities. Provides for vertical and horizontal mixed-use development.

Allowed Land Uses

- + Allows both vertical and horizontal mixed use, and standalone uses.
- + Mixed use: Retail/office, retail/residential, office/residential.
- + Standalone uses: Neighborhood-serving retail, general commercial, services, entertainment, multi-family residential, live/work, and office.



Allowed Development Intensities

- + DU/AC: Up to 60
- + Commercial FAR: Up to 2.0



Streetscape

Streetscapes are urban with formal street tree patterns, wide sidewalks, corner curb-extensions, mid-block crosswalks, and on-street parking in parallel and/or angled configuration.

Connectivity

Block perimeters should not exceed 1,600 feet, which are generally deeper and shorter to accommodate deeper buildings and/or more parking. Intersection density should be at least 400 per square mile.

Open Space

Downtown open space includes corner plazas, courtyards, and in some cases public squares for dining, recreation and special events. Streets and paseos are well-landscaped.

Building Form and Character

Buildings are set close to the street, with shopfronts at the back of sidewalk and residential ground floors set back behind shallow front yards, stoops, porches, and terraces to provide an appropriate degree of privacy for residents. Buildings are two to four stories and contain a mixture of uses.

CENTER

Midtown

Provides of an active, mixed-use center in Midtown by allowing multi-story, mixed-use buildings at higher intensities, community gathering spaces, and land uses to support residents and visitors. Provides for vertical and horizontal mixed-use development.

Allowed Land Uses

- + Allows both vertical and horizontal mixed use, and standalone uses.
- + Mixed use: Retail/office, retail/residential, office/residential.
- + Standalone uses: Neighborhood-serving retail, general commercial, services, entertainment, multi-family residential, live/work, and office



Allowed Development Intensities

- + DU/AC: 10 to 40
- + Commercial FAR: Up to 1.0



Streetscape

Streetscapes are urban with formal street tree patterns, wide sidewalks, corner curb-extensions, often mid-block crosswalks, and on-street parking in parallel and/or angled configuration.

Connectivity

Block perimeters should not exceed 1,600 feet, which are generally deeper and shorter to accommodate deeper buildings and/or more parking. Blocks up to 2,200 feet may be allowed if a pedestrian paseo or greenway is provided near the midpoint. Intersection density should be at least 400 per square mile.

Open Space

In addition to the generous streetscapes and paseos, open space in Neighborhood Centers includes corner plazas, courtyards, and in some cases public squares for dining, recreation and special events.

Building Form and Character

Buildings are set close to the street, with shopfronts at the back of sidewalk and residential ground floors set back behind shallow front yards, stoops, porches, and terraces to provide an appropriate degree of privacy for residents.

CENTER

Neighborhood Center

Provides for areas with a wide variety of neighborhood-serving retail and commercial uses, residential, other amenities, and community-gathering spaces for residents.

Allowed Land Uses

- ✦ Allows both vertical and horizontal mixed use, and standalone uses.
- ✦ Mixed use: Retail/office, retail/residential, office/residential.
- ✦ Standalone uses: Neighborhood-serving retail, general commercial, services, entertainment, multi-family residential, live/work, and office



Allowed Development Intensities

- ✦ DU/AC: 10 to 20
- ✦ Commercial FAR: Up to 0.5



Streetscape

Streetscapes are urban with formal street tree patterns, wide sidewalks, corner curb-extensions, often mid-block crosswalks, and on-street parking in parallel and/or angled configuration.

Connectivity

Block perimeters should not exceed 1,600 feet, which are generally deeper and shorter to accommodate deeper buildings and/or more parking. Blocks up to 2,200 feet may be allowed if a pedestrian paseo or greenway is provided near the midpoint. Intersection density should be at least 400 per square mile.

Open Space

In addition to the generous streetscapes and paseos, open space in Neighborhood Centers includes corner plazas, courtyards, and in some cases public squares for dining, recreation and special events.

Building Form and Character

Buildings are set close to the street, with shopfronts at the back of sidewalk and residential ground floors set back behind shallow front yards, stoops, porches, and terraces to provide an appropriate degree of privacy for residents.

DISTRICT

Regional Commercial

Provides large-format retail development along with associated restaurant and commercial service activity. This district is located near major freeway connections and around major intersections.

Allowed Land Uses

- + Large-format retail, commercial services, lodging, entertainment, restaurant, parks and recreation

Allowed Development Intensities

- + DU/AC: N/A
- + Commercial FAR: 0.35 to 1.0

Streetscape

Streetscapes are urban with formal street tree patterns, wide sidewalks, and on-street parking in parallel, perpendicular and angled configuration. To support large commercial uses, some "blocks" may be devoted entirely to parking.

Connectivity

In pedestrian-oriented retail environments, larger blocks should be subdivided into smaller "virtual blocks" no longer than 400 or 500 feet.

Open Space

Open space is primarily provided through generous streetscapes, outdoor plazas, and neighborhood green spaces.

Building Form and Character

Buildings are entirely near or at the sidewalk to shape the streetscapes. Parking lots may be located on some frontages, provided that they are screened with low walls and/or plantings. Buildings frontages are continuous shopfronts unless broken by courtyards, paseos, or screened parking lots. Service functions, such as loading and trash pickup, should occur at the rear. Buildings can be up to three-stories.



DISTRICT

Workplace and Employment District

Provides for areas with a wide variety of neighborhood-serving retail and commercial uses, residential, other amenities, and community-gathering spaces for residents.

Allowed Land Uses

- + Primary: Light industrial, manufacturing, office, R&D, live/work
- + Secondary: Small retail, commercial services

Allowed Development Intensities

- + DU/AC: N/A
- + Commercial FAR: Up to 0.75

Streetscape

Streets are wider than other areas to accommodate vehicle traffic and trucks, but are still well-landscaped with rows of street trees, wide sidewalks, and on-street parking in parallel, perpendicular, and angled configurations. To support large office uses, some blocks may be devoted entirely to parking.



Connectivity

Larger block sizes are allowed in this district, with perimeters up to 2,600 feet in length. Pedestrian paseos are recommended to help facilitate walking and biking to work.

Open Space

Open space is primarily provided through the generous streetscapes and variety of greens and plazas at key gathering areas. Small- to moderate-sized buildings should consolidate or align shared open spaces. Within larger buildings, open space is provided in the form of courtyards shaded by the buildings.

Building Form and Character

Buildings are located near or at the sidewalk to shape the streetscapes and to frame public open space. In some areas, particularly in order to attract a large employer, it may be appropriate for several blocks to be joined together as a campus environment with pedestrian-only spaces and pathway connecting buildings to one another and to surrounding streets. Buildings may be up to three-stories in height.

DISTRICT

Festival District

Reserves area for outdoor festivals, events, sporting activities and related activities.

Allowed Land Uses

- + Primary: Hospitality, resort, recreation

Allowed Development Intensities

- + Commercial FAR: up to 0.5

Streetscape

For contiguous developed areas, streetscapes have steady rows of street trees planted in parkway strips and generous sidewalks. The typical vehicular design speed is 25 mph.

Connectivity

For contiguous developed areas, block perimeters should not exceed 1,600 feet. Pedestrian paseos are recommended to help facilitate walking and biking within the festival grounds as appropriate.

Open Space

In addition to the generous streetscapes and paseos, open space in Festival District may include corner plazas, courtyards, and in some cases public squares for dining and recreation.

Building Form and Character

Buildings are set close to the street, with shopfronts at the back of sidewalk.



DISTRICT

Public and Institutional District

Reserves areas for public, educational, and institutional uses.

Allowed Land Uses

- ✦ Primary: Civic center, libraries, community centers, senior centers, police stations, fire stations, corporate yards, public and private schools, higher-educational facilities, drainage facilities, public/private recreational facilities, and other similar uses.

Allowed Development Intensities

- ✦ DU/AC: N/A
- ✦ Commercial FAR: up to 0.5 or as determined by City Council.

Streetscape

Streetscapes should be consistent with the character identified by the General Plan placetype in which the public or institutional use is located. If located outside of a placetype, the requirements should be determined on a case-by-case basis.



Connectivity

Connectivity should be consistent with the standards identified by the General Plan placetype in which the public or institutional use is located. If located outside of a placetype, the requirements should be determined on a case-by-case basis.

Open Space

Public and institutional uses are encouraged to provide usable public open space.

Building Form and Character

Public and institutional buildings are set close to the street.

DISTRICT

Parks and Open Space District

Reserves area for public and private recreational use and open space.

Allowed Land Uses

- ✦ Primary: Parks and open space, golf courses, trails, habitat management, passive recreation, amphitheaters, community centers, environmental education and similar uses.

Allowed Development Intensities

- ✦ DU/AC: N/A
- ✦ Non-Residential FAR: up to 0.01

Streetscape

Streetscapes should be consistent with the character identified by the General Plan placetype in which the public or institutional use is located. If located outside of a placetype, the requirements should be determined on a case-by-case basis.

Connectivity

Connectivity should be consistent with the standards identified by the General Plan placetype in which the public or institutional use is located. If located outside of a placetype, the requirements should be determined on a case-by-case basis.

Open Space

Open space is consistent within the types identified in the Parks and Recreation Element of the General Plan.

Building Form and Character

Determined on a case-by-case basis.



DISTRICT

Resource Management and Open Space Overlay

Implements the provisions of the Coachella Valley Multiple Species Habitat Conservation Plan (CVMSHCP) to protect open space and natural resource areas.

Allowed Land Uses and Development Intensities

Allowed land use and development intensity should be consistent with the standards identified by the General Plan placetype in which the parcel is located and any relevant provisions in the CVMSHCP.



Streetscape and Connectivity

Streetscape and connectivity should be consistent with the character and standards identified by the General Plan placetype in which the parcel is located and any relevant provisions in the CVMSHCP.



Open Space

Open space is provided primarily through the preservation and integration of natural features and open space. Open space may be woven into neighborhood design. Primary General Plan park types may include nature / open space, greenways, and trails.

Building Form and Character

Building form and character should be consistent with the provisions identified by the General Plan placetype in which the parcel is located and any relevant provisions in the CVMSHCP. Structures should generally be clustered together to leave most of the natural area undisturbed with groupings of homes organized around natural areas.

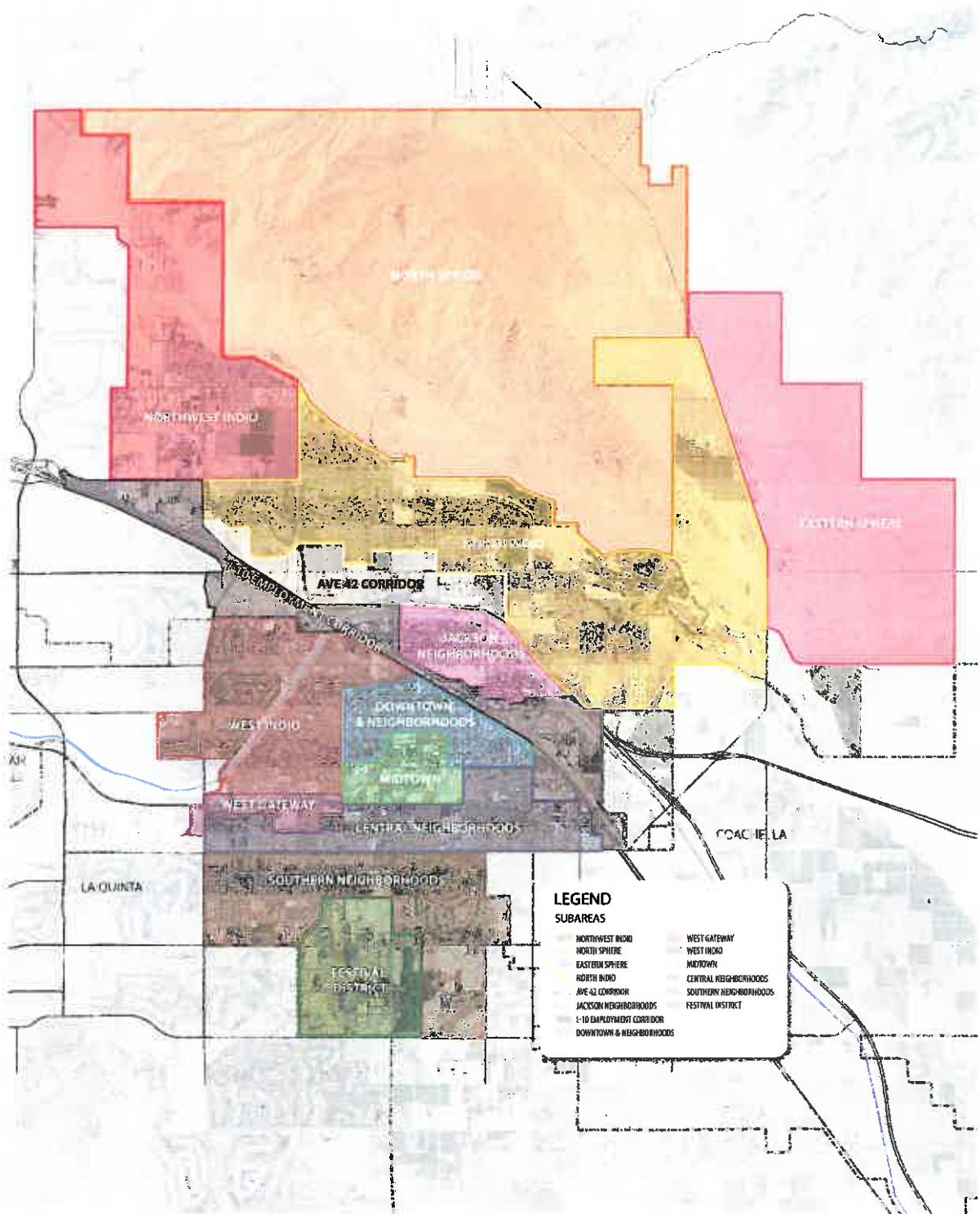
Indio's Subareas

The following section includes a vision and key strategies for specific subareas in Indio, as illustrated in Figure 3-6. Each subarea has unique identifying characteristics and exemplify what makes Indio a desirable place. The location, character, and key issues for these areas are discussed in detail on the following pages, along with strategies that are specific to each. These strategies are in addition to citywide goals and policies discussed later in the Element. It is anticipated that a mix of both citywide and specific subarea strategies will be implemented for each neighborhood.



Top: Lively downtown environment, Bottom: Neighborhood commercial

Figure 3-6: Indio Subareas



Northwest Indio

The Northwest Indio subarea currently includes a mixture of older rural neighborhood development, newer suburban neighborhood development, agricultural uses, and undeveloped open space. It is bounded on the west by golf course development within unincorporated Riverside County, on portions of the south and east sides by suburban residential development within Indio, and the northerly portion of the area is bounded by undeveloped desert to the east, north, and west.

This area offers significant potential for the development of Suburban Neighborhoods and Desert Estate Neighborhoods and resorts. It is recommended that a connected mix of such types be planned for this area, with Suburban Neighborhoods abutting existing similar neighborhoods, transitioning to Desert Estate Neighborhoods abutting the surrounding open desert areas.

Subarea Strategies:

- + Provide greater connectivity in new neighborhoods than is present in the adjoining existing neighborhoods.
- + Provide strong spatial connections between new neighborhoods and adjoining open spaces, with new development facing edge drives along urban and natural open spaces rather than backing toward those spaces.
- + Allow for the development of a Neighborhood Center that would provide access to goods and services to community residents the northwest corner of 39th Avenue and Jefferson Avenue would be a candidate location for this use.
- + Provide a rural edge character along the open desert to north.
- + Plan for an orderly transition from rural land uses to a more urban character over time. Avoid land use incapability when assessing new development proposals.



Neighborhood commercial center



Single family Desert Estates

North Indio

The North Indio subarea is substantially built out with suburban neighborhoods, and with limited exceptions, it is expected that future neighborhood development will follow that same pattern. The primary exception to this is along the Avenue 44 corridor, which is planned as a connective corridor with Connected Neighborhoods. This is a large undeveloped area to the south of Avenue 44, the Citrus Ranch area, and east of the I-10 corridor. Avenue 44 provides a critical connection into the Jackson Neighborhoods to the west, which are already older connective neighborhoods that would benefit from better connection to comparable newer development to the east.

Subarea Strategies:

- + Seek opportunities to connect streets and pedestrian paths to surrounding subareas.
- + Ensure that new centers serve surrounding neighborhoods and are physically accessible via pedestrians and bicyclists.
- + Use specific plan(s) for the large development sites within the neighborhood, requiring coordination and consistency with adjacent specific plans and project master plans, in order to promote an integrated development pattern.
- + Implement clear, strong gateway elements at Golf Center Parkway and Avenue 44.



Walkable neighborhood

Avenue 42 Corridor

The Avenue 42 corridor is one of the top opportunity areas in Indio. This is a key corridor with large areas of undeveloped land on both sides with good freeway access. It is also close to many of Indio's newest neighborhoods.

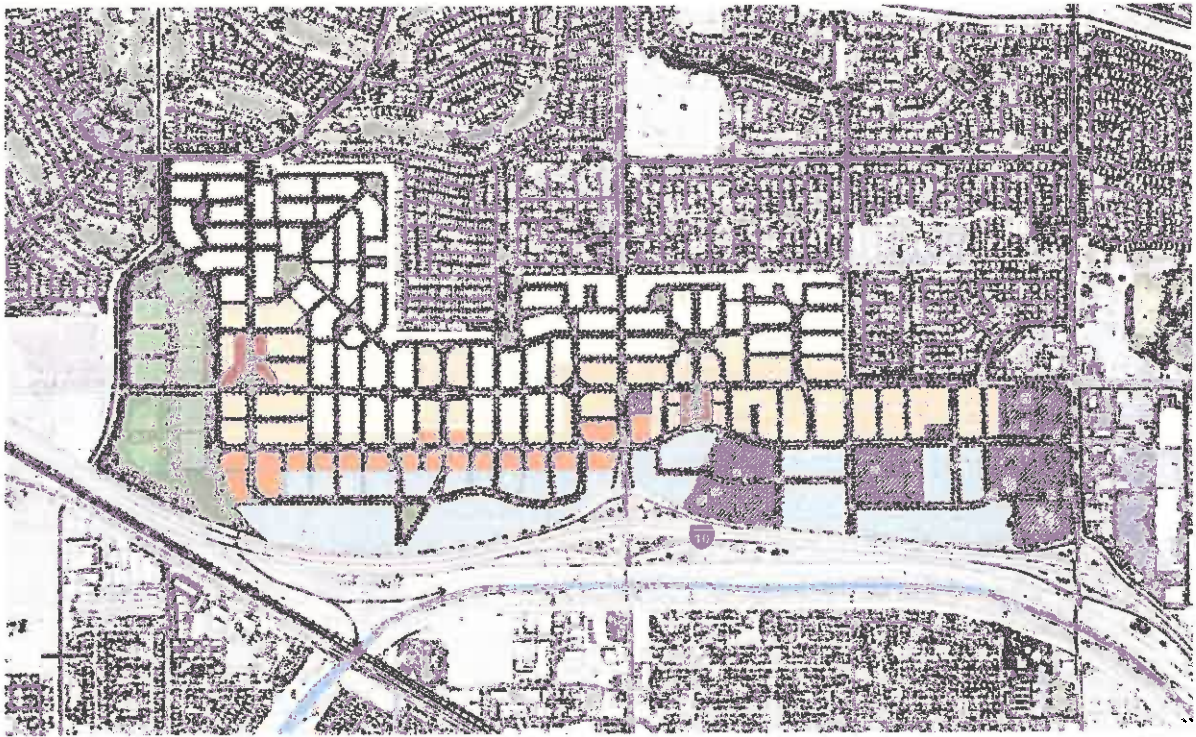
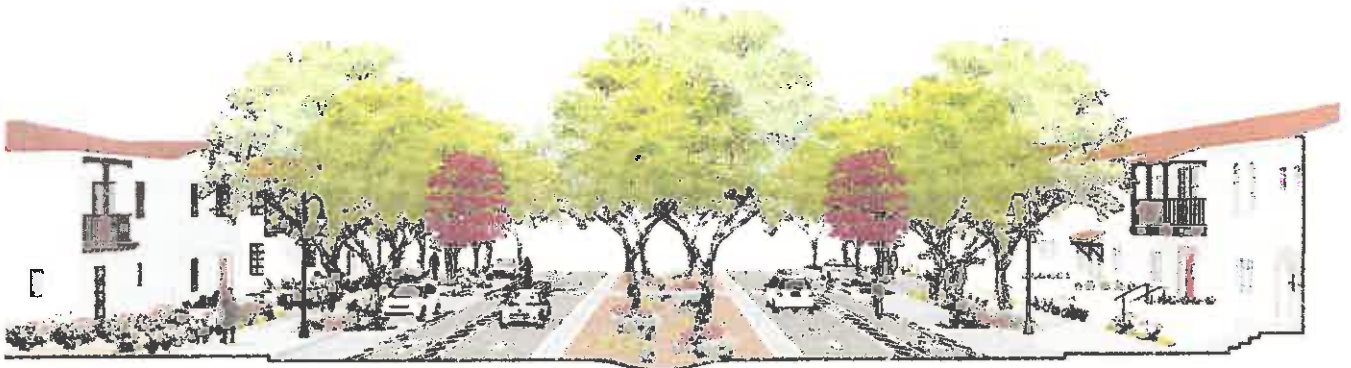
The mix of uses for this area will be largely driven by current and future market trends. The main strategy for capturing and leveraging the value of new investment in the area is to ensure connective development patterns and a strong public realm framework that unifies future neighborhoods, centers, and employment areas into a coherent framework. New development in this area should be planned comprehensively with the final street network and block structure, distribution of uses, and development types and intensities planned for entire neighborhood areas prior to any new development.

An illustrative diagram has been prepared for this area, showing Connective Neighborhoods with a range of low, middle and higher-intensity residential development and Neighborhood Centers at two intersections along Avenue 42. Properties along the south edge of this area are adjacent to the freeway and planned to be a Workplace and Employment District. This area should include an east-west street connected to and backing up to neighborhoods to the north. It will be important that uses be limited to non-noxious light industrial, R&D, and office uses with limited truck traffic.

Subarea Strategies:

- + Require master planned development and large scale comprehensive development for the remaining undeveloped tracts in the area. Consider the use of a specific plan for the large development sites within the neighborhood in order to promote an integrated development pattern.
- + Create a highly-connected, pedestrian-friendly urban form with a fine-grained block and street network and connections to surrounding sub-areas.
- + Design Avenue 42 to balance modes, so bicyclists and pedestrians feel as welcome and safe as motorists.
- + Seek trail connections to parks, open space, and CV link/White Water River channel.
- + Encourage mixed use development along Avenue 42 with a diverse set of land uses on the developable parcels in the corridor.
- + Encourage and assist where practical the creation of a neighborhood center on west side near Madison Street and at Monroe Street.
- + Encourage and assist where practical the redevelopment and infill of retail center at Jackson Street and Avenue 42.
- + Plan for redevelopment and infill of large vacant commercial spaces, such as former Target store.
- + Ensure a variety of housing types and densities in walkable urban formats.
- + Require an extensive tree canopy on streets and parking areas to protect from the summer heat.
- + Create regional recreational area on west side.
- + Implement clear, strong gateway elements at Monroe Street and Jackson Street intersecting Avenue 42 and at I-10 Highway.

Figure 3-7: Example of neighborhood infill along the Avenue 42 corridor



Jackson Neighborhoods

The Jackson subarea is envisioned as vibrant neighborhoods with a variety of housing types for different income levels and household sizes. This area includes older Connective Neighborhoods in need of reinvestment and updated amenities. A major new park is planned for this area, and a Neighborhood Center at Jackson Street and Avenue 44 is envisioned. The strong neighborhood center on Jackson Street will help to meet day-to-day needs of resident’s services and goods. Most infill development should take the form of Connective Neighborhood- Middle, with Connective Neighborhood- High. Jackson Street will be a balanced street that fosters pedestrian and bicycle activity. New quality streetscape design and architecture will ensure this area is a beautiful gateway to City.

Subarea Strategies:

- + Encourage infill residential development to encourage neighborhood cohesion.
- + Update zoning to:
 - o Focus on the pedestrian-oriented building design.
 - o Allow residential uses between major intersections on Jackson Street.
 - o Provide standards for adding second units to existing single family residential units to protect the established neighborhood character.
 - o Support small lot / missing middle housing on small and unusually-shaped lots.
- + Create a streetscape plan within a list of capital improvements for Jackson Street.
- + Consider the use of CDBG funding and other housing rehabilitation program resources in this neighborhood to improve physical infrastructure.



Multi-family residential



Pedestrian-oriented neighborhood commercial

I-10 Employment Corridor

The Interstate 10 corridor consists primarily of industrial land – with fragments of older neighborhoods – along the railroad line and Indio Boulevard (historic Highway 99), and auto-oriented regional commercial uses. These areas developed naturally next to the railroad and Highway 99 early in Indio’s evolution. These areas include a mix of outdated industrial facilities and housing and newer industrial and office facilities.

The major opportunity that this area presents for the future is a “seam” of existing and new jobs in a central location, with good access from the region, the City, and adjoining neighborhoods. The areas would provide for a wide variety of employment types, including, but not limited to, light industrial, manufacturing, logistics, office, and auto mall. Retail and service uses will be integrated into the area to support employers. New developments should be connected to, but buffered from, existing and future neighborhoods on adjacent properties.

Subarea Strategies:

- ✦ Encourage business parks, research and development, and office uses within the area, while connecting this district to adjoining existing and future neighborhoods for multi-modal worker access.
- ✦ Encourage the siting of light industrial, manufacturing, and logistics uses within the corridor that creates employment/job opportunities. Establish buffers between incompatible uses.
- ✦ Encourage the expansion and retention of auto sales and related activities along I-10 near Varner Road.
- ✦ Site regional retail opportunities that reduce retail leakage to neighboring communities and attract visitors in this neighborhood.
- ✦ Collaborate with the local tribal governments to facilitate development of retail opportunities in the Fantasy Springs District that would complement their hospitality development and attract new visitors from their casino and entertainment venues.
- ✦ Recognizing that this is the core of Indio’s non-retail employment generating land uses, encourage development that is complementary to these activities.
- ✦ Prioritize businesses that provide jobs for a range of skill level and businesses that manufacture goods and provide services locally and regionally.
- ✦ Systematically improve streetscape to attract new businesses and investment and to encourage and promote the full range mobility options. Require active-transportation connections and facilities to adjacent neighborhoods.
- ✦ Encourage limited retail uses to support employment.
- ✦ Implement clear, strong gateway elements on Jackson Street.



Highway oriented commercial



Flexible employment space

West Indio

West Indio is largely built out with stable suburban neighborhoods. This area is not expected to undergo significant change in the future. Improvements envisioned for this area may include streetscape enhancements to improve safety and comfort for pedestrians and bicyclists, and aesthetics and environmental beautification through addition of street trees. Undeveloped or underutilized properties along major corridors, such as Madison Street, Miles Avenue, and Fred Waring Drive, present opportunities for new neighborhood-serving commercial uses proximate to existing neighborhoods.

Subarea Strategies:

- + Implement secondary gateway elements at Miles Avenue and Fred Waring Drive.
- + Encourage mixed use development with a diverse set of land uses on the developable parcels along Madison Street, Miles Avenue, and Fred Waring Drive and at the intersection of Monroe Street and Indio Boulevard.
- + Implement spur connections to the CV Link.
- + Provide standards for adding second units to existing single family residential units to protect the established neighborhood character.
- + Identify improved pedestrian and bicycle connections to Midtown and Downtown.
- + Consider the use of community facilities districts, specifically landscape lighting and maintenance districts in the neighborhood to support the development of connected pedestrian friendly environments that encourage commercial activity.



Mixed use center

West Gateway

This important entry to Indio from La Quinta lies along the south edge of the West Indio planning area and presents a significant opportunity for commercial, hospitality, and residential development. The vision for the area is a vibrant, pedestrian-oriented corridor that serves as welcoming gateway to Indio. Highway 111 is transformed into a multi-way boulevard to facilitate necessary automobile capacity as well as provide for safe, pleasant pedestrian and bicycle environment. New, frequent north-south streets connect existing and future neighborhoods to Highway 111. With a pedestrian-oriented design, a highly-connected street network, a connected open space network, and a mix of uses, the West Gateway subarea has the opportunity to connect seamlessly to Midtown and become a key gateway into the City.

Subarea Strategies:

- + Use a specific plan or master plan (or zoning code update) for the large development sites within the neighborhood in order to promote an integrated development pattern that addresses urban land use, streets and mobility, public realm network, and open space network.
- + Plan for Highway 111 as a multi-way boulevard to facilitate necessary automobile capacity as well as provide for safe, pleasant pedestrian and bicycle environment. Establish public realm and streetscape requirements that provide shade for pedestrians and strong vertical element to help minimize automobile speeds.
- + Create multiple, closely-spaced pedestrian connections across Highway 111.
- + Support master planned development that allows for integration into the existing urban fabric for new projects along Highway 111, and creates a coherent, walkable, transit-serviceable subarea with a comprehensive network of complete streets.
- + Allow mixed-use development along Highway 111, balancing residential, office, and retail / service uses.
- + Encourage and recruit for commercial and hospitality uses as part of any mixed used proposals in the neighborhood.
- + Provide for medium density residential in a walkable format behind frontage parcels.
- + Implement clear, strong gateway elements at Highway 111 and Jefferson Street.



Gateway commercial

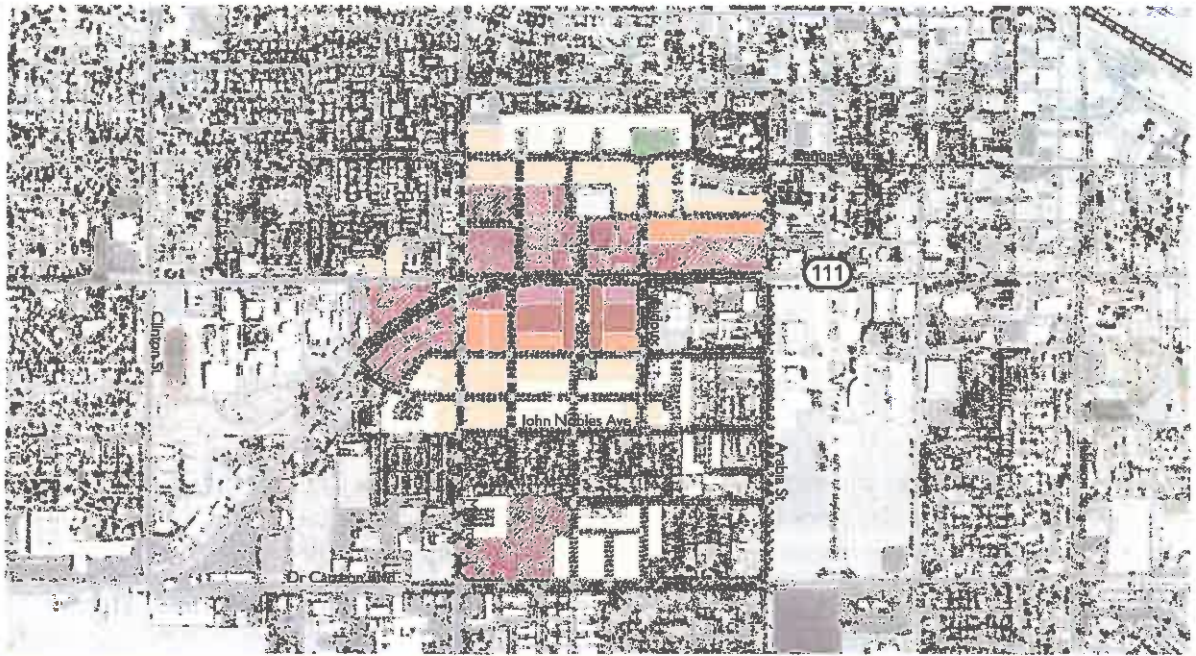
Midtown

The Midtown subarea represents a significant opportunity (in tandem with the Downtown) for the creation of a strong and centered civic identity for Indio. Centered on the past-its-prime Fashion Mall, including many large vacant parcels of land, this area on Highway 111 is positioned to be reinvented as the working center of town. Close to and connected to the historic Downtown and County Fairgrounds – and at the center of town at Monroe Street and Highway 111 – this subarea can provide a vital mix of neighborhood and community-serving retail, office/employment, hotel/hospitality, restaurants, multi-family housing, and single family housing. Encourage Highway 111 to be transformed into a multi-way boulevard to facilitate necessary automobile capacity as well as provide for safe, pleasant pedestrian and bicycle environment. New, frequent streets subdivide large parcels, create valuable new addresses, and connect existing and future neighborhoods to Highway 111 and surrounding neighborhoods.

Subarea Strategies:

- ✦ Use a specific plan or master plan (or zoning code update) for the large development sites within the neighborhood in order to promote an integrated development pattern that addresses urban land use, streets and mobility, public realm network, and open space network.
- ✦ Consider seeking a master developer to lead large-scale redevelopment efforts that are envisioned for the district. Support the plan with available municipal resources to ensure high-quality development and public realm improvements from the earliest phases to completion.
- ✦ Implement a placemaking program transitioning from a short-term lighter, quicker, cheaper strategy to longer-term fixed placemaking solutions.
- ✦ Provide support funding within Downtown and along the Highway 111 corridor to encourage the establishment of property-based business improvement districts (BIDs) among interested property owners that would provide marketing, enhanced local services, and a common voice to work in partnership with the City's economic development staff.
- ✦ Work with local stakeholders to spur revitalization of older neighborhoods of the City. Target public, cultural, recreational, and infrastructure implementation actions within the neighborhood.
- ✦ Explore providing incentives aimed at recruiting and retaining specific businesses, such as theaters, hotels, and sit-down restaurants that support active and sustained pedestrian and community use in the subarea.
- ✦ Facilitate mixed-use residential development primarily within the Downtown, along the Highway 111 corridor, and near public transit to create additional demand for retail and dining opportunities.
- ✦ Work cooperatively with property owners including the Indio Fashion Mall owners and surrounding properties to facilitate redevelopment of this area as a regional retail destination with residential and mixed-use development, building upon its central location in the community along Highway 111.
- ✦ Implement a Midtown marketing and brand strategy.

Figure 3-8: Illustration of Midtown Infill Center and Neighborhoods



Mixed use, neighborhood centers, gateway center

Downtown and Neighborhoods

Downtown Indio and the neighborhoods to the east and west represent the historic core of Indio. The General Plan – and parallel Downtown Specific Plan – envision the revitalization of the downtown as the civic and cultural heart of the community. The downtown is envisioned as a vibrant, mixed-use center of the City with residential, educational, civic, entertainment, office, and retail uses. The vision is also to reestablish passenger rail service, to reinvest in adjoining neighborhoods, and to reinvent the Highway 111 corridor. The downtown will be well connected to a new Midtown area and the adjacent neighborhoods.

Subarea Strategies:

- + Support the implementation of the Downtown Specific Plan
- + Lead local businesses, property owners, the arts community, and the College of the Desert in the revitalization of Downtown as the central meeting place for living, working, dining, entertainment, civic, and cultural activities.
- + Provide support funding within Downtown and along the Highway 111 corridor to encourage the establishment of property-based business improvement districts (BIDs) among interested property owners that would provide marketing, enhanced local services, and a common voice to work in partnership with the City's economic development staff
- + Collaborate with the staff of Riverside County's expanded Downtown Government Center to anticipate the demand for future retail, dining, and professional service needs on adjacent or nearby sites.
- + Implement a placemaking program transitioning from a short-term lighter, quicker, cheaper strategy to longer-term fixed placemaking solutions.
- + Align infrastructure investments and the capital improvement program (CIP) with the aim of creating a walkable pedestrian-oriented character in the subarea. Maintain all existing streets, and with new development, require new paseos, pathways, bikeways, and walkways to increase connectivity.
- + Reduce surface parking by lower parking requirements, unbundling park (unbundling involves separating the cost to rent a parking space from the cost to rent an apartment), and establishing a shared parking system.
- + Work with local and regional transit agencies to assess and eventually establish passenger rail service to the Downtown.



Pedestrian oriented downtown and paseo

Central Neighborhoods

These areas lie just to the south of the Highway 111 corridor and the Midtown, West Gateway, and Downtown subareas. The Central Neighborhoods are characterized by single-family suburban housing development. This area is not expected to undergo significant change in the future. Rather the area will preserve the existing housing stock. Improvements envisioned for this area may include streetscape enhancements to improve safety and comfort for pedestrians and bicyclists, and to better connect the neighborhoods to Midtown, Downtown, and Highway 111. Streetscape improvements may also include aesthetic and environmental beautification. New neighborhood-serving commercial uses are also encouraged through infill development on opportunity sites on major streets and avenues.

Subarea Strategies:

- + Identify active transportation and transit options and develop implementation strategies.
- + Encourage infill residential development to encourage neighborhood cohesion.
- + Cluster medical uses around JFK Hospital and adjacent medical uses.
- + Consider the use of CDBG funding in this neighborhood to improve physical infrastructure.



Single family residential neighborhoods



Class II bikeway



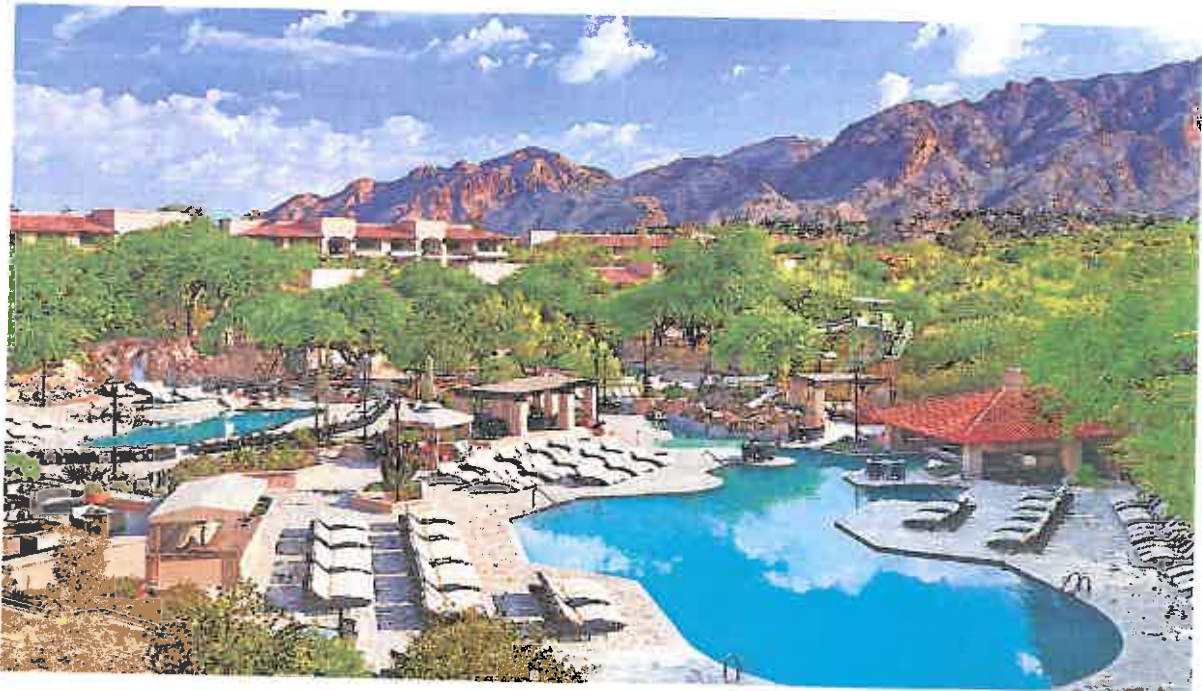
Multi-family residential neighborhood

Southern Neighborhoods

These neighborhoods lie between the Central Neighborhoods and the Festival District, presenting the opportunity for a rural/resort lifestyle with easy access to the commercial and civic amenities of Midtown and Downtown and the entertainment offerings of the Festival District. This quiet, secluded living environment close to Indio's arts and entertainment hubs (the Festival grounds) provides these neighborhoods with a unique value proposition and can help support this Plan's primary goals for supporting the success of those important centers of community life.

Subarea Strategies:

- + Keep neighborhoods stable.
- + Improve multi-modal access to Midtown and the Festival District
- + Keep 20-acre minimum project master plan requirement in place for new development to maintain quality and cohesiveness.
- + Allow for uses that can benefit and create synergies with activities and events in the Festival District.



Resort

Festival District

Indio's Festival District represents an unlikely and unique resource for the community – a private initiative that has put this City on the international entertainment map. The success of this venue springs from its combination of isolation and accessibility. As it matures from occasional concerts and special events to a more permanent venue, there are opportunities for surrounding it with carefully-considered hospitality and visitor-serving facilities and resort-oriented housing.

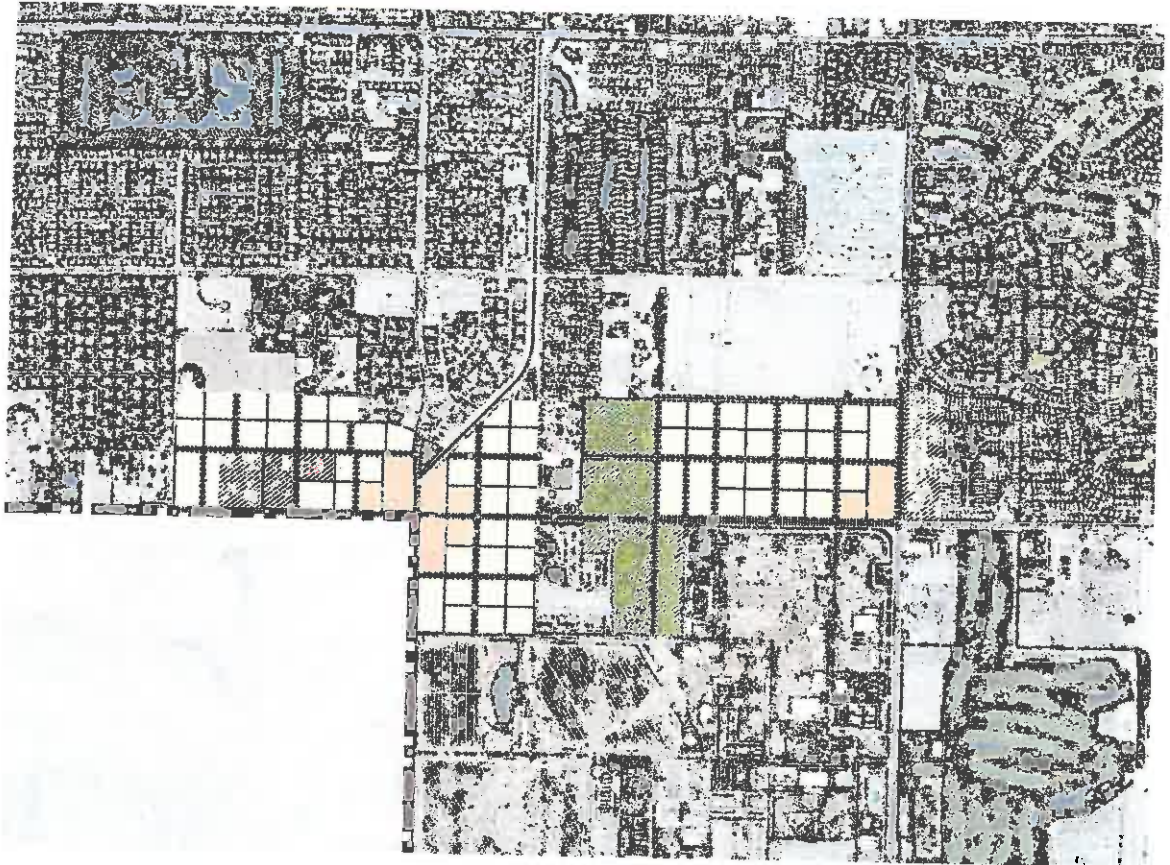
Subarea Strategies:

- + Strictly limit uses of surrounding properties to uses that are directly supportive of, or not in conflict with, festival-resort atmosphere.
- + Provide all-mode connectivity between new hospitality and resort development to the festival grounds to promote a safe, active and attractive environment for visitors. Preserve and enhance access to and from District.
- + Retain 20-acre minimum project master plan requirement in place for new development to help maintain a high-quality and cohesive design character. Incentivize smaller properties to coordinate with adjacent properties. Consider the development of a coordinated Festival District Strategy.
- + Consider hospitality support services, small-scale service retail, and resort-oriented housing that add to a cohesive festival district.
- + Protect the festival grounds from encroachment by potential land use incompatibilities, recognizing the highly variable and intense use of the festival grounds.
- + Align capital improvement program (CIP) with infrastructure needs that support ongoing festival operations.
- + Work with festival operators and promoters to capture as much secondary and off site economic activity as possible. In particular work on maximizing transient-occupancy taxes (TOT) and other local revenues that could be generated by activities at the Festival grounds.
- + Encourage festival and event operators to utilize local labor from the community whenever possible. Consider negotiating community benefits agreements as part of any extraordinary permitting.



Coachella Valley Music and Arts Festival

Figure 3-9: Illustration of Festival District Infill



Northern Sphere

This subarea contains the northern portion of Indio's sphere of influence. The area is largely passive and active open space, housing a limited amount of existing commercial development and resource extraction. The Northern Sphere is considered a long-term planning reserve area. It may be considered for future urban expansion, including Desert Estates, when and if the capacity of Indio's existing incorporated area is substantially exhausted. Annexation must not create a revenue drain on the City. Much of the subarea is covered by the Coachella Valley Multi-Species Habitat Conservation Plan (CVMSHCP). Hillside views from Indio should be maintained.

Subarea Strategy:

- + Support active and passive recreational opportunities in the area.
- + Implement the Coachella Valley Multi-Species Habitat Conservation Plan.
- + Before allowing annexation, create a land use plan and conduct a related fiscal analysis that demonstrates that annexation will be revenue neutral or revenue enhancing.
- + Strongly limit the type, location, and nature of new development, in compliance with CVMSHCP.



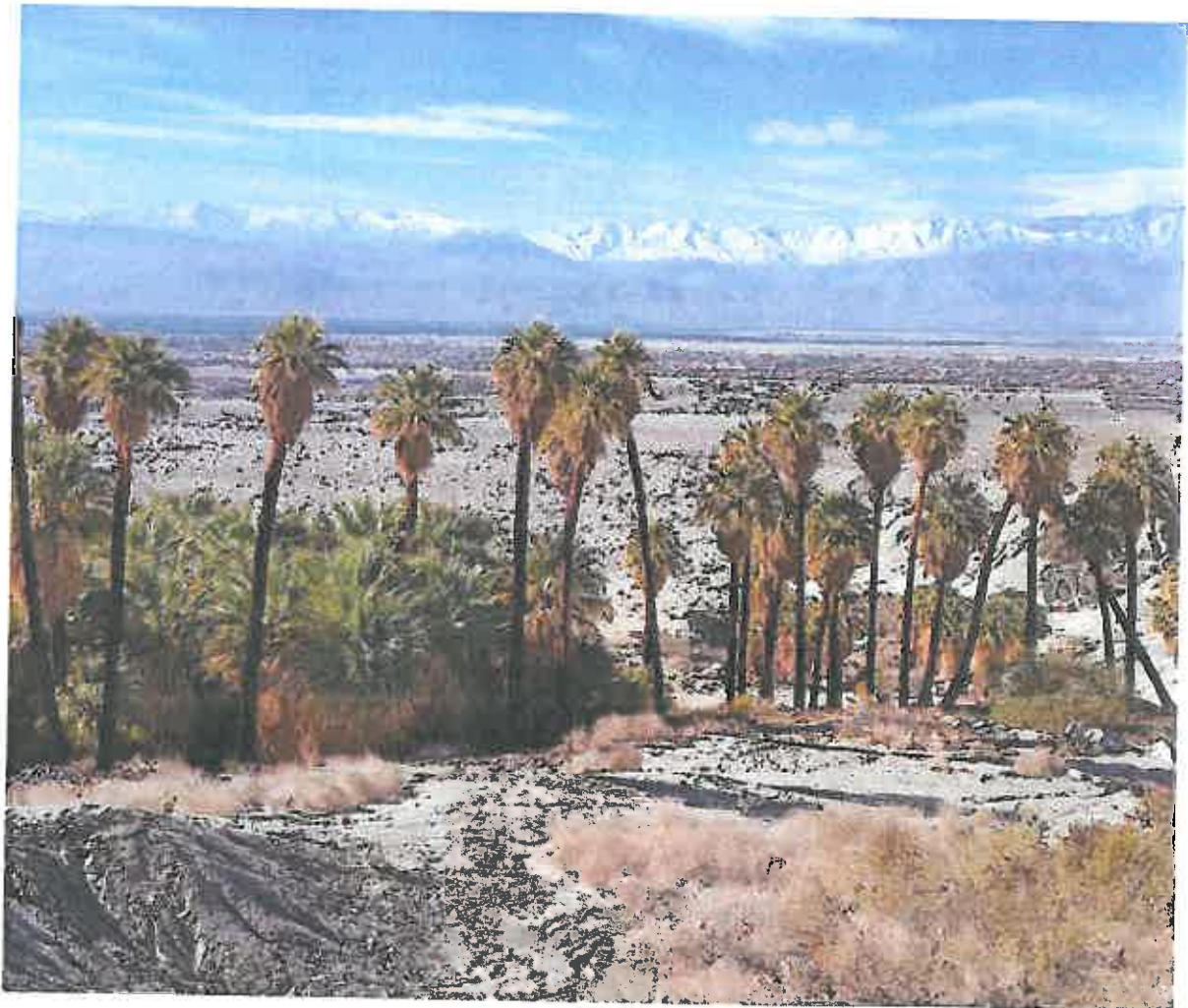
Hiking trails in Indio

Eastern Sphere

This subarea contains the eastern portion of Indio's sphere of influence. The area is largely passive open space, housing a limited amount of existing commercial development and resource extraction. The Eastern Sphere is considered a long-term planning reserve area. It may be considered for future urban expansion, including Desert Estates, when and if the capacity of Indio's existing incorporated area is substantially exhausted. Annexation must not create a revenue drain on the City. Some of the subarea is covered by the CVMSHCP. Hillside views from Indio should be maintained.

Subarea Strategy:

- + Support active and passive recreational opportunities in the area.
- + Implement the Coachella Valley Multi-Species Habitat Conservation Plan.
- + Before allowing annexation, create a land use plan and conduct a related fiscal analysis that demonstrates that annexation will be revenue neutral or revenue enhancing.
- + Strongly limit the type, location, and nature of new development, in compliance with CVMSHCP.



Indio's natural open space

Goals and Policies

Urban Form

Indio will offer an appropriate mix of housing, employment, retail and services, recreation, arts, education, and entertainment for the City's residents and businesses. It will grow responsibly and sustainably in a manner which benefits the community now and into the future.

Goal LU-1: Citywide Urban Structure. An urban structure that enhances the quality of life of residents, meets the community's vision for the future, and weaves new growth areas together with established Indio neighborhoods.

LU-1 Policies

LU-1.1 Overall City Structure. Establish a clearly defined City structure by:

- + Re-establishing the City's pedestrian-oriented Downtown as a community anchor with local and regional-serving civic, arts, education, and entertainment uses.
- + Transforming the Midtown area into a mixed-use center with retail, commercial services, and residential uses in a walkable format.
- + Maintaining and enhancing the Festival District as a key location for large-scale entertainment and recreational opportunities.
- + Creating mixed-use corridors along Highway 111, Monroe Street, Avenue 42, and Avenue 44 that contain a mix of retail, service, office, and residential uses. Corridors should have defined nodes that provide a mix of local- and regional-serving uses.
- + Investing in existing residential neighborhoods adjacent to Downtown and Midtown.
- + Infilling new residential neighborhoods arranged around neighborhood centers and community gathering spaces, such as schools and parks.
- + Maintaining the I-10 Employment Corridor subarea as a critical economic engine for the City.
- + Facilitating major, regional-serving commercial districts that provide a mix of commercial, entertainment, and service uses in a pedestrian-oriented format north of I-10 at Jackson Street and Avenue 40 to capitalize on regional transportation and access.
- + Preserving and protecting natural open space and conservation areas in the Northern and Eastern Sphere subareas.

LU-1.2 Infill First. Prioritize initial capital improvements and other public investments and guide private investments into the Downtown, Midtown, Jackson Neighborhood, and Avenue 42 Subarea first to limit expansion of the City's urban footprint.

LU-1.3 Contiguous Development. When development occurs outside the Downtown, Midtown, and existing central neighborhoods, locate new development adjacent to Indio's built environment to create a contiguous expansion of the City.

LU-1.4 Connecting New and Old. Connect new growth areas with existing Indio neighborhoods through transportation investments, open space connectivity, wayfinding, and urban design strategies.

LU-1.5 Subareas. Implement the strategies identified for each distinct subarea in Indio in this General Plan.

LU-1.6 Bermuda Dunes Airport Capability. Indio shall work to achieve consistency between General Plan land use and the Airport Comprehensive Land Use Plan, as is appropriate for the community. Measures may include restrictions on permitted land uses and development criteria, including height, land use type, and intensity.

Public Realm

Indio will offer pleasant, safe, and enjoyable public spaces. The following goals and policies strive to ensure Indio's public realm serve the community.

Goal LU-2: Active Places. Indio is a City with active and comfortable places that encourage social interaction and community gathering.

LU-2 Policies

- LU-2.1 Walkable Neighborhoods.** Require all new neighborhoods to be pedestrian friendly by including features, such as short blocks, wide sidewalks, shaded streets, buildings that define and are oriented to streets or public spaces, traffic-calming features, convenient pedestrian street crossings, and safe streets designed for pedestrians, cyclists, and vehicles.
- LU-2.2 Balanced Land Uses.** Maintain a balanced land use pattern to support a broad range of housing choices, retail businesses, employment opportunities, educational and cultural institutions, entertainment spaces, and other supportive uses and within long-established Indio neighborhoods and new growth areas.
- LU-2.3 Access to Amenities.** Strive to create development patterns such that the majority of residents are within one-half to one-mile walking distance of a variety of neighborhood-serving uses, such as parks, grocery stores, restaurants, places of worship, cafes, dry cleaners, laundromats, banks, hair care, pharmacies, civic uses, and similar uses.
- LU-2.4 New Gathering Spaces.** Require new developments to provide public parks, plazas, and squares that establish interesting gathering spaces in planned districts and neighborhoods. Require project developers to establish mechanisms, such as a Community Facilities District, to adequately maintain new parks, recreational facilities, and infrastructure.
- LU-2.5 Existing Gathering Spaces.** Improve existing gathering spaces throughout the City to provide attractive, comfortable, and inviting public and pedestrian spaces, encouraging walking and public gathering spaces.
- LU-2.6 Shade.** Require the use of large shade trees or structures to provide comfortable environments during hot months.
- LU-2.7 Parking Lot and Prominent Pathways.** Require trees, shade structures, and/or other cooling methods to provide comfortable walkways during hot months.

Goal LU-3: Human-Scaled Public Realm. A City designed for people, fostering interaction, activity, and safety.

LU-3 Policies

- LU 3.1 Streetscape Design.** Create pedestrian-oriented streetscapes by establishing a unified approach to street tree planting, sidewalk dimensions and maintenance, pedestrian amenities, and high-quality building frontages.
- LU 3.2 Pedestrian Focus on High Volume Streets.** Design the streetscape of high volume corridors to balance regional traffic flow with safe and convenient pedestrian movement.
- LU 3.3 Parking Frontages.** Require parking design standards that ensure parking areas do not dominate street frontages and are screened from public views whenever possible.
- LU 3.4 Tree Planting.** Encourage the planting of trees that appropriately shade the sidewalk and off-street parking areas to improve the pedestrian experience throughout the City.
- LU 3.5 Lighting.** Require all new street lights in commercial areas to be pedestrian-oriented and scaled, attractively designed, compatible in design with other street furniture, and to provide adequate visibility and security in accordance with best practices for night sky protection.
- LU 3.6 Public Plazas.** Encourage new development to incorporate public plazas, seating, drinking fountains, and gathering places, especially in prominent locations and areas of pedestrian activity.
- LU 3.7 CPTED.** Use Crime Prevention through Environmental Design strategies (CPTED) in new and existing development to improve public safety, including the following strategies:
- + Active public space
 - + Building design to promote “eyes on the street”
 - + Clear delineation between private and public space
 - + Natural access control between public and private space
 - + Maintenance of public places
 - + Removal or repair of vandalism or broken property

Goal LU-4: High-Quality Building Design. A beautiful city with a high-quality architecture and building design.

LU-4 Policies

- LU-4.1 Quality Design.** Use simple, urban building forms made with permanent materials with high-quality detailing that stands the test of time.
- LU-4.2 Scale and Articulation.** Use building organization and construction to derive scale and articulation rather than surface ornamentation.
- LU-4.3 Building Materials.** Convey façade articulation through the strength, depth, and permanence of building materials. Thinner cladding materials, such as stucco, stone and masonry veneers, and wood or simulated wood, may be used when finished to appear as durable and authentic of the materials they simulate.
- LU-4.4 Building Entrances.** Use visual and physical design cues within a building’s design and entries to

emphasize the building entrance and connections to public spaces.

LU-4.5 Iconic Design. Allow iconic and memorable building designs, particularly on larger non-residential properties.

LU-4.6 Climate-Appropriate Design. Encourage the use of building techniques and materials that relate to Indio's warm and dry desert climate. Promote solar control and use of shade in building design and associated pedestrian amenities.

LU-1.5 Protect Visual Characteristics. Protect Indio's unique visual characteristics and views.

Goal LU-5: Connected Places. A network of transportation corridors throughout the city that provides a high level of connectivity for vehicles, bicyclists, and pedestrians.

LU-5 Policies

LU-5.1 Complete Street Projects. Design, plan, maintain, and operate streets using complete street principals for both new infrastructure and for retrofit/maintenance projects. This includes reviewing the entire right of way and prioritizing modes as noted on Figure 4-1.

LU-5.2 Street Connectivity. Encourage short block spacing for new development to enhance connectivity to neighborhoods. In key areas of the City, work with existing land owners to improve connectivity for bicycles and pedestrians.

LU-5.3 Complete Street Context. Ensure that complete street applications integrate the neighborhood and community identity into the street design.

LU-5.4 Subarea Connectivity. Ensure a high-level of connectivity in all Neighborhoods, Centers and Districts throughout the City. The connectivity should be measured as block perimeter or length and in external connectivity on the perimeter of a new development project.

LU-5.5 Connections between Development Projects. Require the continuation of the street network or pedestrian connections between adjacent development projects and discourage the use of cul-de-sacs except where necessary or due to existing development, topographic conditions or limited access to transportation systems.

LU-5.6 Improved Connections. Improve pedestrian and bicycle mobility by identifying opportunistic connections within the City's neighborhoods to increase access to local parks, schools, neighborhood centers, and neighborhood gathering spaces.

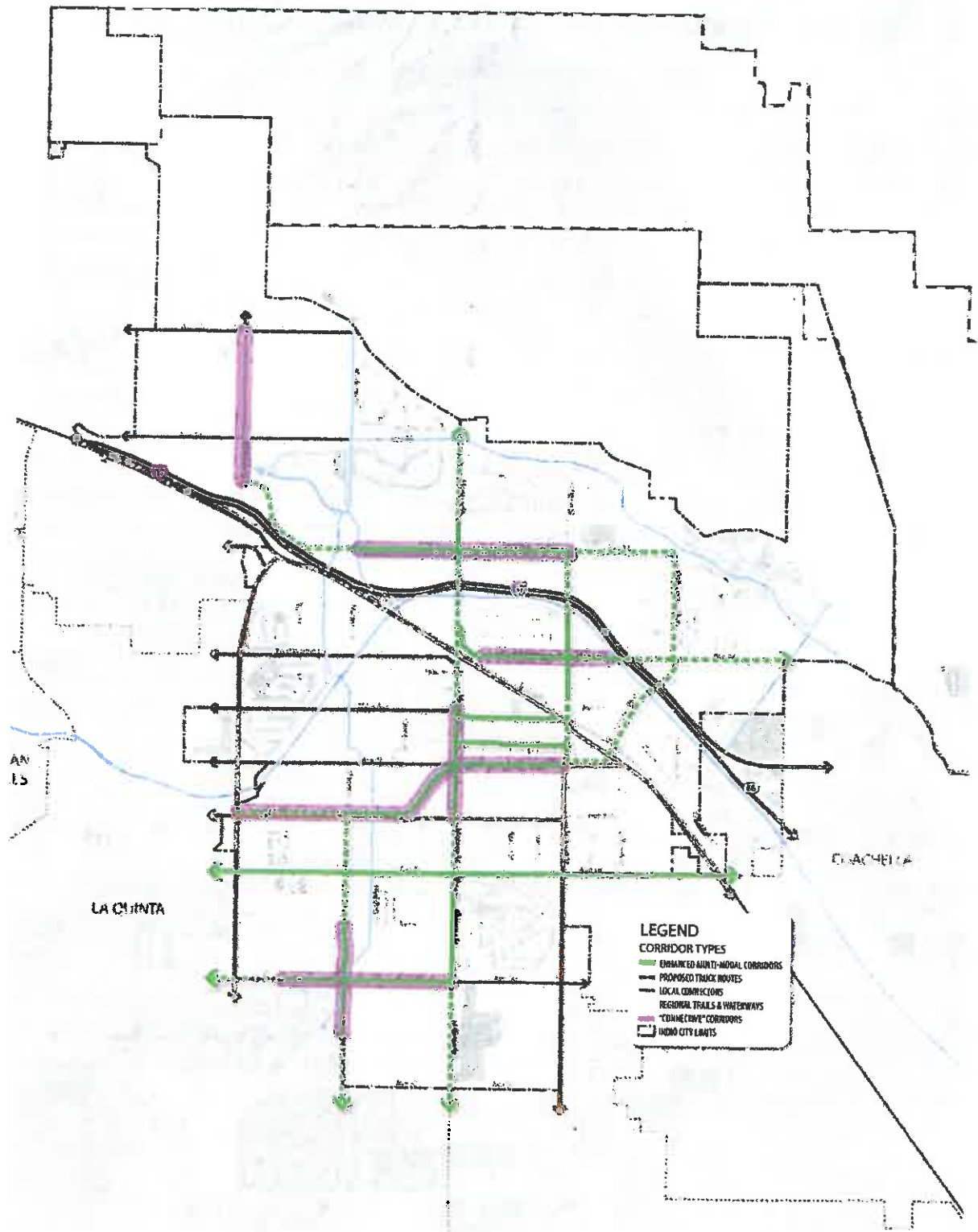
LU-5.7 Pedestrian-Supportive Building Design. Require new and substantially rehabbed commercial and mixed-use projects to follow best practices for pedestrian-supportive design:

- + Ensure pedestrian orientation of ground floor uses in new development.
- + Place primary building facades and entrances near the front property line or back of sidewalk. In limited cases allow small plazas and active landscaped areas for social gathering between the building and sidewalk.
- + Scale building elements to pedestrians.
- + Design new buildings along corridors to provide for a rear building transition between the primary arterials and any adjacent low-density residential neighborhoods.

- + Require parking internal to buildings that faces primary arterials or side streets to use appropriate design (such as faux facades, green walls, public murals, etc.) to minimize its visual impact.
- + Require that new development include wide sidewalks, trees, pedestrian furniture, safe pedestrian crossings and direct connections to the front entrances of retail and services.
- + Encourage new commercial developments to have common driveways to minimize the number of curb cuts along any given block to improve pedestrian safety.
- + Screen trash and recycling collection areas, loading facilities, mechanical equipment, outdoor storage areas, utility connections, and antennas from public view.
- + Encourage covered parking

LU-5.8 Connective Corridors. Ensure high-quality, people-oriented street design and urban design occurs where highlighted by the Connective Corridors in Figure 3-10.

Figure 3-10: Connective Corridors



Neighborhoods

Indio will support existing stable, livable neighborhoods, and new walkable neighborhoods that provide a variety of housing types, densities, and designs in close proximity to amenities.

Goal LU-6: Enhance Existing Neighborhoods. A City with well-maintained residential neighborhoods that support Downtown and Midtown.

LU-6 Policies

- LU-6.1 Maintenance.** Support the on-going maintenance and improvement of existing residential properties; in particular, encourage property owners to maintain and improve their front yards and building facades.
- LU-6.2 Orientation.** Orient all new residential construction towards streets, public spaces, or shared private spaces, placing parking to back or side of the lot.
- LU-6.3 Sustainable Residences.** Encourage sustainable building practices during new construction or when buildings are substantially renovated.
- LU-6.4 Home Businesses.** Support home businesses that meet City planning and permitting requirements and create jobs and opportunities for entrepreneurship.
- LU-6.5 Blight and Nuisances.** Work with neighborhood associations, commissions, and other community groups to proactively identify and address nuisances and eliminate blight.
- LU-6.6 Open Space Maintenance.** Strengthen public-private partnerships with local neighborhood organizations and homeowners associations to build and maintain neighborhood parks and recreational facilities.
- LU-6.7 Compatible Scale.** Maintain high-quality existing residential neighborhoods by ensuring new development projects and infill construction are of a compatible scale and provide adequate transitions to adjacent residential properties.
- LU-6.8 Second Units.** Permit second units on single-family residential lots.
- LU-6.9 Building Additions.** Allow additions, such as a new bedroom or bathroom that are complementary to the existing character of the neighborhood. Renovations and expansions should use complementary building materials and forms while allowing for flexibility for unique design conditions.
- LU-6.10 High-quality Landscaping and Fencing.** Encourage property owners to maintain and improve their yards and the front facades of homes and to encourage the use of drought-tolerant landscaping. Prohibit front yard fences made from concrete blocks or chain links.
- LU-6.11 Circulation Connectivity.** Seek opportunities to enhance and maintain existing residential neighborhoods by improving pedestrian and bicycle facilities, installing traffic calming measures, and “punch through” cul-de-sacs.

Goal LU-7: New Neighborhoods. Neighborhoods that provide a variety of housing types, densities, designs and mix of uses and services that support healthy and active lifestyles.

LU-7 Policies

LU-7.1 Complete Neighborhoods. Through the development entitlement process, ensure that all new Neighborhoods (areas with a “Neighborhood” General Plan Designation) are complete and well-structured such that the physical layout and land use mix promote walking to services, biking and transit use, are family friendly, and address the needs of multiple ages and physical abilities. New neighborhoods should have the following characteristics:

- + Contain short, walkable block lengths.
- + Contain a high level of connectivity for pedestrians, bicycles and vehicles where practicable.
- + Organize around a central focal point such as a park, school, civic building or neighborhood retail such that most homes are no more than one quarter-mile from this focal point.
- + Have goods and services within a short walking distance.
- + Contain a diversity of housing types, where possible.
- + Have homes with entries and windows facing the street.
- + Have a grid or modified grid street network (except where topography necessitates another street network layout).
- + Provide a diversity of architectural styles.

LU-7.2 Balanced Neighborhoods. Within the allowed densities and housing types, promote and allow for a range of housing and price levels within each neighborhood in order to accommodate diverse ages and incomes. For development projects larger than five acres, require that a diversity of housing types be provided and that these housing types be mixed rather than segregated by unit type.

LU-7.3 Housing Affordability. Ensure affordable housing is distributed throughout the City to avoid concentrations of poverty and to be accessible to jobs.

LU-7.4 Block Size. Require new neighborhoods to be designed with blocks length ranging from 400 or less to 800 feet consistent with the General Plan place type. Exceptions can be made if mid-block pedestrian and bicycle connections are provided.

LU-7.5 Provision of Sidewalks. Except within designated Rural Estate areas, require sidewalks of at least five feet in width on at least one side of private and both sides of public streets in neighborhoods and prohibit obstructions, such as utility boxes, telephone poles, street signs, etc. that would impede sidewalk use.

LU-7.6 Tree-lined Streets. Require trees on both sides of at least 60 percent of new and existing streets within the project and on the project’s side of bordering streets, between the vehicle travel way and walkway at intervals averaging no more than 50-100 feet (excluding driveways and utility vaults). This standard shall apply whenever new streets are constructed or when existing streets and sidewalks are significantly rehabilitated with existing neighborhoods. Ensuring the appropriate revenue stream to provide long-term maintenance.

- LU-7.7 Shaded Sidewalks.** Require shade over at least 50 percent of the length of sidewalks on streets within a project. Trees must provide shade within five-years of landscape installation and should be as water efficient as possible.
- LU-7.8 Gated Communities.** Discourage the construction of new gated communities. When gated communities are allowed, require frequent pedestrian and bicycle connections between the gated community and surrounding areas at distances no more than 600 feet apart.
- LU-7.9 Soundwalls.** Allow the use of soundwalls to buffer new neighborhoods from existing sources of noise pollution, such as railroads and limited-access roadways. Prohibit the use of soundwalls to buffer residential areas from arterial, boulevard, connector, Downtown, and local streets. Instead design approaches such as building setbacks, landscaping, and other techniques shall be used. In the case where soundwalls might be acceptable, require regular pedestrian access points at intervals less than 600 feet to improve access from the neighborhoods.
- LU-7.10 Subdivision Gateways.** Strive for neighborhoods to blend seamlessly with one another and discourage distinguishing one residential project from another.
- LU-7.11 Connections to Key Destinations.** Require direct pedestrian and bicycle connections between residential areas and nearby commercial and public and institutional areas as well as other residential areas.
- LU-7.12 Access to Parks and Open Spaces.** Require the design of new neighborhoods and, where feasible, retrofit existing neighborhoods, so that the majority of dwelling units are within a half-mile walking distance of a usable open space such as a tot-lot, neighborhood park, community park, or plaza/green.
- LU-7.13 Density Bonuses.** Support projects that seek for density bonuses for affordable housing, special-needs housing, childcare, and senior housing.
- LU-7.14 Unbundled Parking.** Require unbundled parking, reducing or eliminating requirements for on-site parking in the Downtown and Midtown subareas.

Goal LU-8: Resort Uses. Encourage resort uses within neighborhoods and centers that enhance the quality of places.

LU-8 Policies

- LU-8.1 Resort Uses.** Allow hospitality, recreation, single and multi-family residential with golf course activity or similar recreational orientation, small-scale retail and services, and similar uses which are compatible with and oriented towards serving the needs of resort-oriented living.
- LU-8.2 Resort Neighborhoods.** Permit resort uses in any General Plan placetype with a neighborhood designation. Resorts should reflect the overall character, allowed uses, and intensity of the placetype.
- LU-8.3 Resort Connectivity and Streetscape.** Require streetscape design and street connectivity be consistent with the character and standards identified by the General Plan placetype in which the resort parcel or project is located. Pedestrian access to perimeter streets or adjoining neighborhoods or districts should be provided approximately every 600 feet to encourage walking, biking, or equestrian activity.

LU-8.4 Gated Resorts. Allow resort uses to be gated, at the discretion of the City. When gated communities are allowed, require frequent pedestrian and bicycle connections between the gated community and surrounding areas at distances no more than 600 feet apart.

Centers and Districts

Indio will work to attract new businesses to the community, increase resident and visitor access to shops and services, and foster a distinctive character that enriches the City's image and identity to support and empower the community.

Goal LU-9: Centers. A variety of mixed use, urban centers throughout the City that provide opportunities for shopping, recreation, commerce, employment, and arts and culture.

LU-9 Policies

LU-9.1 Regional Destination. Establish Downtown Indio as regional destination with a mix of uses that draw visitors from the Coachella Valley. Encourage a broad mix of uses that creates an inviting, active, and dynamic Downtown including:

- + Local- and regional-serving retail and services
- + Cultural, arts, and institutional uses
- + Educational uses and facilities
- + Small, local-serving office uses
- + Residential buildings to increase housing diversity and units
- + Innovative, public spaces and parks

LU-9.2 Neighborhood Center Design. Design new neighborhood centers to be walkable and pedestrian-friendly with buildings that face streets and public sidewalks. Primary building entrances should face the street.

LU-9.3 Access to Transit. Encourage the development of Neighborhood Centers that are located on existing or planned transit stops, particularly along Highway 111, Avenue 42, and Avenue 50, in order to facilitate and take advantage of transit service, reduce vehicle trips, and allow residents without private vehicles to access services.

LU-9.4 Pop-up Uses. Support transformation of vacant or under-utilized properties and buildings for pop-up uses including restaurants, art spaces, and cultural facilities, among other uses.

LU-9.5 Parking Districts. Create special parking districts in Downtown and Midtown to encourage shared parking and that can be used to fund and facilitate creation of park once facilities or other non-auto transportation improvements.

LU-9.6 Unbundled Parking. Require unbundled parking, reducing or eliminating requirements for on-site parking in the Downtown and Midtown subareas.

LU-9.7 Incentives for Aggregation. Allow or create incentives, such as density bonuses, parking reductions, setback reductions, or other regulatory modifications, for projects that aggregate parcels to create a larger contiguous parcel.

Goal LU-10. Workplace and Employment Districts. Employment-oriented districts provide space for industrial, manufacturing, office, and research and development in order to increase access to jobs and enhance the fiscal stability of the City.

LU-10 Policies

LU-10.1 Design of Employment Districts. Design Workplace and Employment Districts to be urban in character with compact buildings sited at or near front lot lines, provide a high percentage of lot coverage, and building facades and entrances directly facing the street. Buildings should have a high degree of transparency and visibility. Parking lots and large setbacks shall not dominate the frontage of the Workplace and Employment Districts.

LU-10.2 Emerging Industries. Support a diverse mix of light industrial, information, maker, and technology uses in order to provide jobs and tax revenues for the community by allowing emerging economic uses and industries within the Workplace and Employment designation.

LU-10.3 Flex Spaces. Expand the number of flex facilities on land designated as Workplace and Employment to accommodate technology, light manufacturing, and service tenants and diversify the City's economic base.

LU-10.4 Non-polluting Industries. Promote development of non-polluting industries that are not major sources of air and water pollution or other negative externalities.

LU-10.5 Industrial Compatibility. Where industrial uses are near existing and planned residential development, require that industrial projects be designed to limit the impact of truck traffic, air and noise pollution on sensitive receptors.

LU-10.6 Employee Services. Enhance the working environment within the Workplace and Employment District by promoting small retail areas and restaurants to serve employees.

LU-10.7 Walkable Blocks. Add new street connections to create walkable blocks to improve pedestrian safety and access consistent with the streetscape and connectivity standards defined for the placetype.

LU-10.8 Transit Accessible. Work with Sunline, area employers, and other partners to provide frequent, quality transit service to Workplace and Employment Districts.

Goal LU-11. The Arts. A high quality of life and strong community identity enhanced by rich arts and culture programs.

LU-11 Policies

LU-11.1 Performing Arts. Support the establishment of allowed land uses and facilities that embrace and celebrate the cultural diversity and rich history of Indio and that promote and encourage arts and entertainment.

LU-11.2 Culture and Heritage. Enhance community culture and heritage through the establishment of public art, themed street furniture, signage, banners, landscaping, special paving, and lighting.

LU-11.3 Development Projects. Require development projects provide public artwork or contribute to an Art in Public Places fund.

LU-11.4 Public Art. Continue to require art in public places to be located in an area that either highlights the design of the building or center or is incorporated within an on-site pedestrian plaza.

LU-11.5 Art Interaction. Support public art installations that encourage interaction with the public such as works that can be touched, sat on, walked on, etc.

LU-11.6 Decorative Gateways. Celebrate the gateways to Indio, Downtown, Midtown, and other prominent destinations by enhancing them with the integration of public art by local artists.

LU-11.7 Artist Live/Work Studios. Encourage live/work studio space in new Downtown developments, where appropriate.

LU-11.8 Public Infrastructure. Encourage integration of art into new and rehabilitated public infrastructure, such as streets, sidewalks, intersections, interchanges, etc.

LU-11.9 Community arts programs. Support transformation of vacant or under-utilized properties and public buildings for cultural and arts-based programs to support area revitalization.

Infrastructure

Working in conjunction with project applicants, Indio will ensure adequate infrastructure and services for residents, businesses, and visitors.

Goal LU-12: Infrastructure Provision. Existing and future residents and businesses are adequately serviced by public services and infrastructure.

LU-12 Policies

LU-12.1 Annexation. Before allowing annexation in the Northern or Eastern Sphere of Influence subarea, create a land use plan and conduct a related fiscal analysis that demonstrates that annexation will be revenue neutral or revenue enhancing. Annexation shall not create a revenue drain on the City.

LU-12.2 Adequate Infrastructure. Ensure adequate infrastructure and utility service levels before approving new development.

LU-12.3 Fair Share. Require new development to pay their fair share for required improvements to public services and infrastructure.

LU-12.4 Actual Costs. Ensure that development impact fees are updated often and reflect the actual costs of improvements.

LU-12.5 Phasing of Public Facilities. Require new parks, open spaces, and public facilities be constructed concurrent with, or prior to, the development of each residential neighborhood. All required parks, open spaces, and public facilities shall be constructed before a majority of the dwelling units are constructed.

LU-12.6 Facilities Districts. Require project developers to establish mechanisms, such as a Community Facilities District, to adequately maintain new parks and recreational facilities and other shared resources.

Implementation Actions

Implementation Programs

The table below identifies programs, policy updates, planning efforts, coordination efforts, and other actions that will help implement the General Plan’s land use and urban design vision and policies. Programs are consistent with this chapter’s goals and policies.

Table 3-3: Land Use and Urban Design Programs

	DESCRIPTION	PRIORITY	TIME FRAME	RESPONSIBILITY
1	<p>Specific Plan Consistency. Following adoption of the General Plan, review the existing, adopted Specific Plans and Project Master Plans and make changes to ensure consistency with the General Plan.</p>	High	Medium	Planning
2.	<p>Update zoning. Modify zoning standards to promote neighborhood, district, and center design. Zoning standards should consider building intensity, height and massing, placement on a lot, parking location and amount, ground-floor open space, and parking location and access. The revised zoning should include:</p> <ul style="list-style-type: none"> • Form-based regulations and standards that create more certainty in the development review process, and clearly define walkable centers and neighborhoods. Design guidelines to illustrative community character and complement development requirements. • Building design standards to foster growth and development in the existing residential neighborhoods, including by-right up zoning and support for missing middle housing. • Updated subdivision ordinance. • Streamline desired uses to permit uses that foster greater economic opportunity and conform to General Plan by-right, including hospitality, visitor-serving uses, and temporary or pop-up uses. • Allow for reduced parking requirements, unbundled parking, shared parking, and multiple tenant structures that encourage development of shared spaces and services. 	High	Short	Planning

3	<p>Development Review. Work to find efficacies and make development permitting as simple and as transparent as possible.</p> <ul style="list-style-type: none"> • Move elements of permitting process on line where possible • Integrate the permitting and planning with GIS capabilities to aid in program development • Work to reduce multiple visits per applicant 	Medium	Ongoing	Planning
4	<p>Neighborhood master planning process. Review existing project master plan requirements, and update the planning process to enable incremental development of vacant parcels while generating interconnected centers, neighborhoods, and employment areas of the City.</p>	High	Short	Planning, Public Works
5	<p>Development fees. Update citywide development impact fees for infrastructure, affordable housing, other community benefits, and long-range planning.</p>	High	Medium	Planning, Economic Development, Public Works, Parks
6	<p>Business Improvement Districts. Facilitate the formation of Business Improvement Associations (BIA), Business Improvement Districts (BID), or similar to unite business owners into area branding, marketing, on-going maintenance, and beautification efforts. Focus initial efforts in Downtown, Midtown, and Highway 111.</p>	High	Medium	Planning, Economic Development
7	<p>Placemaking program. Implement a placemaking program focused on cost-effective and flexible activities, such as a “lighter, quicker, cheaper” approach in Midtown and Downtown.</p>	High	Short	Planning, Economic Development
8	<p>Midtown marketing. Create a Midtown branding and marketing strategy.</p>	Medium	Short	Planning, Economic Development
9	<p>Midtown Master Developer. Study whether to seek a master developer to lead redevelopment efforts in the Midtown subarea.</p>	High	Short	Planning, Economic Development
10	<p>Development Monitoring. Monitor future development and growth in the City and report annual changes to the Planning Commission and City Council. Key metrics may include: housing by type and income level, commercial floor area, jobs, vehicle miles traveled, and greenhouse gas emissions.</p>	Medium	Ongoing	Planning
11	<p>Infill housing incentive program. Establish an infill housing incentive program consistent with the Housing Element. Potential incentives may include an expedited building permit process, impact fee waivers, or other incentives.</p>	Medium	Long	Planning, Economic Development
12	<p>General Plan Reviews. Review the General Plan on a five-year cycle, including a review of individual elements and community programs. Periodically revise the General Plan and respond to individual Plan Amendment petitions.</p>	High	Short	Planning, Economic Development





chapter four

MOBILITY

The purpose of the Mobility element is to create a transportation network for the City that balances model priorities to address the safe and efficient operation, maintenance, and management of the circulation network. The goals and policies in the element have been developed to ensure that all streets within the City are reviewed through a “complete streets” lens – meaning that all streets should provide safe accommodation for all users of the transportation network.

This approach does not mean that all streets should equally prioritize all modes of travel nor does it imply that one mode should dominate street planning on all streets within the City. Rather, different modes should be prioritized on different streets, providing a layered network of streets that responds to the context of the adjacent land use.

Key Considerations and Strategies

Mobility serves as a key component of a City. City-owned streets represent the single largest asset for a City and need to be planned for accordingly. However, streets need to be properly sized to meet the user's demand. Making streets unnecessarily wide increases the City's capital and maintenance costs, increases vehicle speeds, and increases pedestrian exposure crossing the roadway. While under sizing a roadway can create congestion and increase travel times for vehicles and deliveries. The goal of this Element is to develop a street network that balances the needs of all users.

The City's vision for the future will only be realized by ensuring that each new public improvement builds towards that future. The following strategies shall guide public and private investments in the transportation to achieve the vision and are reflected in the General Plan mode priority map, Street Plan, and policy framework.

- ✦ **Implement Complete Streets:** The City will provide a safe, efficient, and accessible transportation system that serves the mobility needs of all users of all ages and abilities. Additional details on complete streets are provided in the following section.
- ✦ **Advocate for Active Transportation:** One of the City's goal is to provide a network of bicycle and pedestrian infrastructure that supports the City's complete street efforts and provides connectivity to existing local and regional facilities, activity centers in the City, employment areas, and parks, open space, and trails like CV Link.
- ✦ **Support Transit:** The City will work with SunLine Transit and RCTC to analyze and forecast commuter traffic trends to develop strategic ways to make a more efficient transit system.
- ✦ **Maintain Levels of Service:** The City will identify and maintain existing roadway levels of service, specifically high-volume roads or main corridors in the City's transportation network.
- ✦ **Actively Oversee Goods Movement and Airport Compatibility:** The City will maintain a safe and efficient system for delivering goods and services, specifying truck routes on City streets, and shall maintain compatibility with the Bermuda Dunes Airport.
- ✦ **Manage Parking:** The City will pursue the "right sizing" of off-street parking within the City by encouraging shared parking, reducing parking requirements, and unbundling parking in key subareas.
- ✦ **Integrate Transportation Network Companies (TNCs) and Autonomous Vehicles (AVs):** This City will plan for and support an increase in use of TNCs and AVs.
- ✦ **Manage Traffic:** The City will support the safe and efficient movement of goods and services by providing signal synchronization and to allow optimum flow in automobile prioritized corridors.

Several State laws as well as regional and local planning efforts provide context related to planning the transportation system. Appendix M describes these key efforts in greater detail.

Complete Streets

The National Complete Streets Coalition (which is a subsection of Smart Growth America) defined complete streets as:

“Complete Streets are streets for everyone. They are designed and operated to enable safe access for all users. Pedestrians, bicyclists, motorists, and public transportation users of all ages and abilities are able to safely move along and across a complete street. Complete Streets make it easy to cross the street, walk to shops, and bicycle to work. They allow buses to run on time and make it safe for people to walk to and from train stations.”

The National Complete Streets Coalition also identifies the following key ten components that make up a comprehensive complete streets policy:

- + Includes a vision for how and why the community wants to complete its streets.
- + Specifies that “all users” includes pedestrians, bicyclists and transit passengers of all ages and abilities, as well as trucks, buses and motor vehicles.
- + Applies to both new and retrofit projects, including design, planning, maintenance, and operations, for the entire right of way.
- + Makes any exceptions specific and sets a clear procedure that requires high-level approval of exceptions.
- + Encourages street connectivity and aims to create a comprehensive, integrated, connected network for all modes.
- + Is adoptable by all agencies to cover all roads.
- + Directs the use of the latest and best design criteria and guidelines while recognizing the need for flexibility in balancing user needs.
- + Directs that Complete Streets solutions will complement the context of the community.
- + Establishes performance standards with measurable outcomes.
- + Includes specific next steps for implementation of the policy.

A complete streets vision is more than implementation of a State-mandated approach. It is a fundamental shift in how the City will plan and design the street system – recognizing the street as a public space and ensuring that the public space serves all users of the system (elderly, children, bicycles, pedestrians, etc.) within the urban context of that system (e.g. accounting for the adjacent land uses).

The Mobility Element is consistent with and further enhances the State and federal requirements for complete streets. Complete streets recognize that each street within the City is unique given its geographic setting, adjacent land uses, and the desired use of that facility. As such, the Mobility Element identifies the City’s transportation network, and based on the uniqueness of the street and surrounding land uses, determines which modes of travel (pedestrian, bicycle, vehicles, etc.) should be prioritized on that street.

Typically, pedestrian streets have slow vehicle travel speeds, short-distance pedestrian crossings, and include some type of buffer between the vehicle travel way and the pedestrian walkway. Streets that prioritize automobiles may have wide travel lanes, multiple turn lanes, and higher travel speeds.

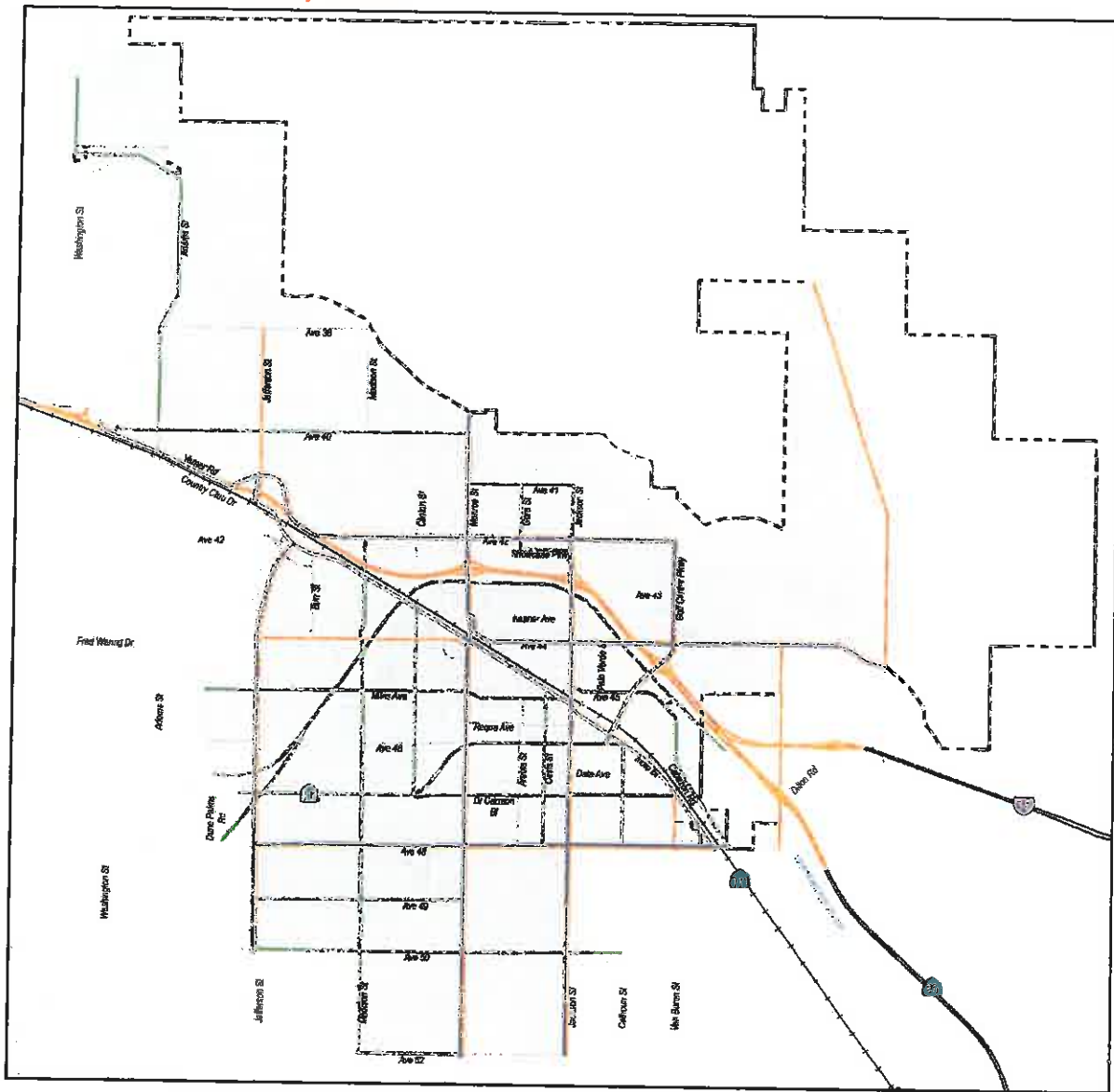
The complete streets implementation utilizes the Institute of Transportation Engineer’s guidance related to a “layered networks” approach. This approach identifies preferred travel modes for each street in the City. Non-preferred travel modes are accommodated along the street, but their service is not prioritized. The priorities of some of these streets may shift 10 to 20 years into the future. If the desire for transit or other active modes of transportation increases, more streets could move from auto-prioritized to prioritizing other modes.

Figure 4-1 depicts the City’s layered network complete street system. Figure 4-6 identifies the City’s complete streets strategy for prioritizing modes based on street typology.



Example of a complete street transformation along Avenue 44 (existing street above, complete street below).

Figure 4-1: Mode Priority Streets



Mode Priority Streets



- City Limits
- Sphere of Influence
- Roads
- Railroads
- Highways
- Truck Routes
- Automobile Priority
- Bicycle Priority Streets

Source: Riverside County/LAFCD (City Boundary, 2012; SDU012); Riverside County/TMA (Roads/Railroads/Highways); Riverside County (River)

The information on this map was derived from various digital databases sourced above. Care was taken in the creation of the map but it is provided "as is". PDC cannot accept any responsibility for any errors, omissions, or positional accuracy, and therefore, there are no warranties which accompany this product. Users are cautioned to verify information on the product before making any decisions.

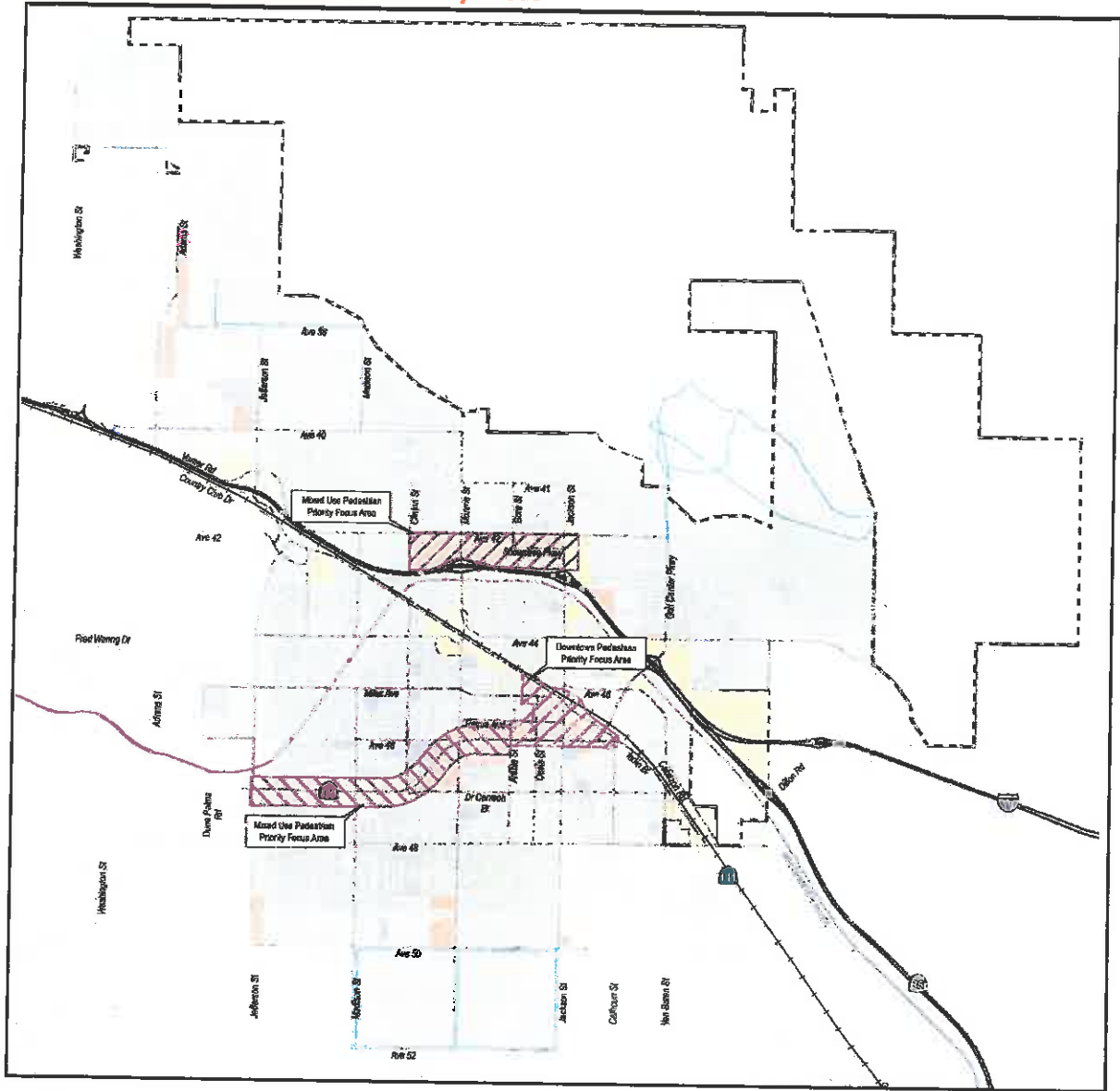
Pedestrian Network

Pedestrian environment affects all residents and visitors—whether driving, bicycling, or riding transit, most people are a pedestrian at some point in their journey. Adequate pedestrian infrastructure, together with land uses that promote pedestrian activities, can help increase walking as a means of transportation, recreation, as well as exercise, and can consequently provide significant health benefits to those who are able to participate.

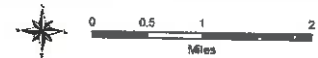
Pedestrian facilities (i.e., sidewalks, crosswalks, trails) are a key component of a multi-modal transportation system, which should enable people of all age groups and abilities to safely walk to their destinations. These facilities help connect and provide access to schools, shopping, employment, transit stations, and other amenities.

This mobility element identifies three key areas of the City as Pedestrian-Priority Areas as shown on Figure 4-2. In these areas, the City will look to minimize the number of lanes on the roadway, reduce vehicle travel speeds, and provide shade and separation for pedestrians.

Figure 4-2: Planned Pedestrian-Priority Areas



Planned Pedestrian Priority



- City Limits
- Sphere of Influence
- Roads
- Railroads
- Highways
- Continental Crosswalk with Lead Pedestrian Interval
- Sidewalk Infill
- Trails
- Existing School
- Recreation, Parks and Open Space
- Mixed Use
- Commercial
- Planned Transit Center

(Source: Riverside County/LAFCO (City Boundary 2012; SOL 5012); Riverside County/TLMA (Roads, Railroads, Highways); Riverside County (River))

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Bicycle Network

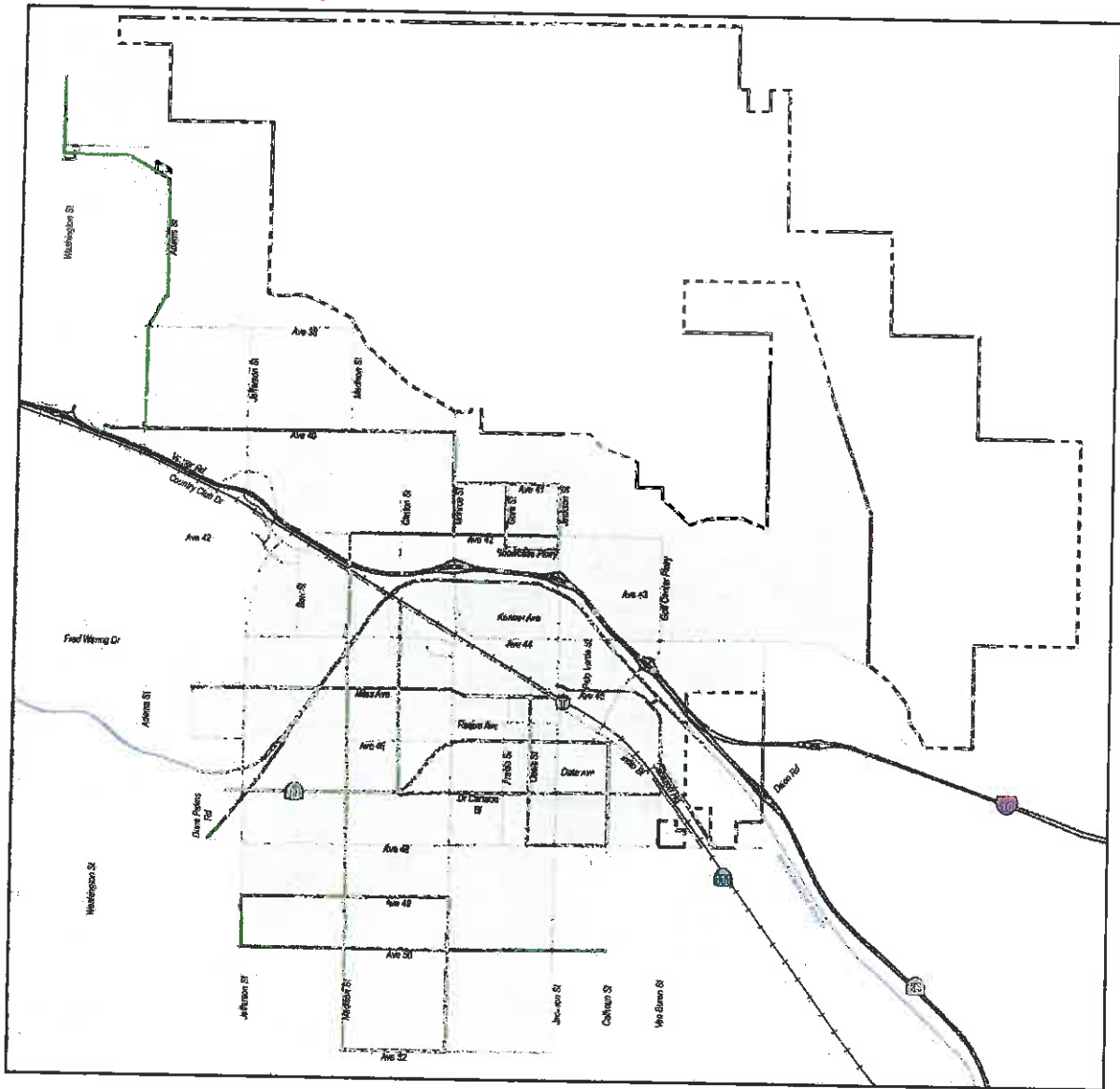
A comprehensive, safe, and well-maintained bikeway network with supporting facilities has multiple benefits. It can help to increase the mode share for bicycling, reducing the number of short vehicle trips by shifting those trips to bicycling helps improve circulation, and reduce greenhouse gas emissions. This mobility element identifies a high-quality bicycle network that includes Class I, II, and IV bicycle facilities to support active transportation, as shown on Figure 4-3.

The City is also beginning a Complete Streets Master Plan which will provide additional guidance on this topic. Although that process will further refine the City's preferred bicycle network, a preliminary bicycle network has been defined and is presented on Figure 4-5 to supplement the bicycle priority facilities noted above.



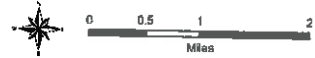
Example of a Class II bikeway.

Figure 4-3: Bicycle Priority Streets



Bicycle Priority Streets

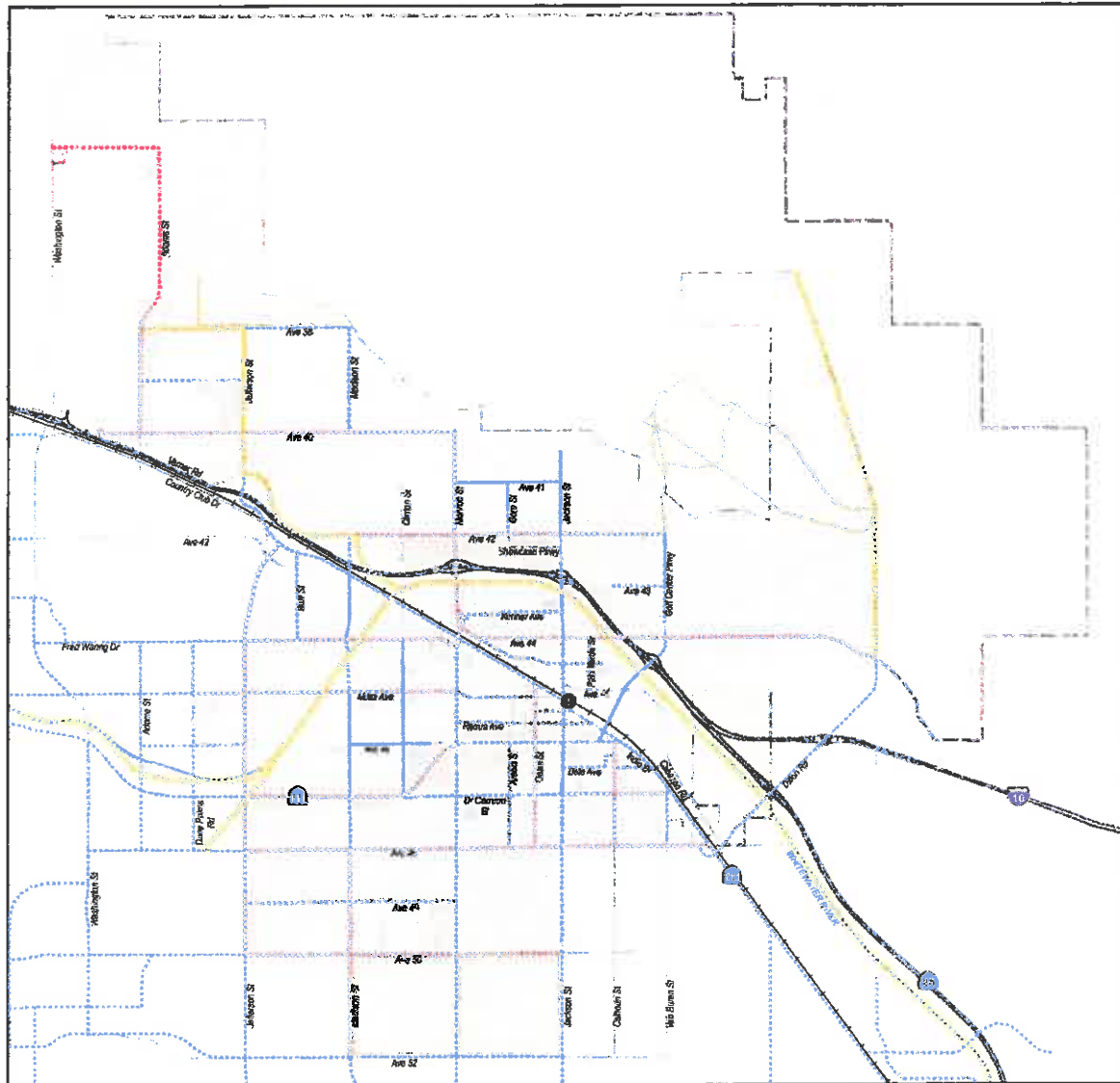
- City Limits
- Sphere of Influence
- Roads
- Railroads
- Highways
- Bicycle Priority Streets



Source: Riverside County/LAFCD (City Boundary, 2012; SDL2012); Riverside County/TLAA (Riverside Railroads/Highways); Riverside County (Rivers)

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Figure 4-4: Planned Bicycle Network



Planned Bicycle Network



- | | | |
|---------------------|---------------------------|----------------------------------|
| City Limits | Class 1 - Bike Path | Existing School |
| Sphere of Influence | Class 2 - Bike Lane | Mixed Use |
| Roads | Class 3 - Bike Route | Commercial |
| Railroads | Class 4 - Cycle Track | Recreation, Parks and Open Space |
| Highways | Proposed Bicycle Facility | Planned Transit Center |
| | Trails | |

Source: Riverside County LA-CD (City boundary 2012, SOI 2012)
Riverside County TMA (Roads, Railroads, Highways) Riverside County (Bicy)

The information on this map was derived from various digital databases, as noted above. Care was taken in the creation of this map but it is provided "as is". PDG cannot accept any responsibility for any errors, omissions, or positional inaccuracy and therefore there are no warranties which accompany this product. Users are cautioned to refer to primary information on this product before making any decisions.

Caltrans has defined four primary bikeway facility types. These facility types are described below:

- + **Class I Bike Path.** Provides a separated corridor that is not served by streets and highways and is away from the influence of parallel streets. Class I bikeways are for non-vehicle use only with opportunities for direct access and recreational benefits, right-of-way for the exclusive use of bicycles and pedestrians, and cross flow conflicts are minimized. CV Link will be a Class I facility connecting the Coachella Valley.

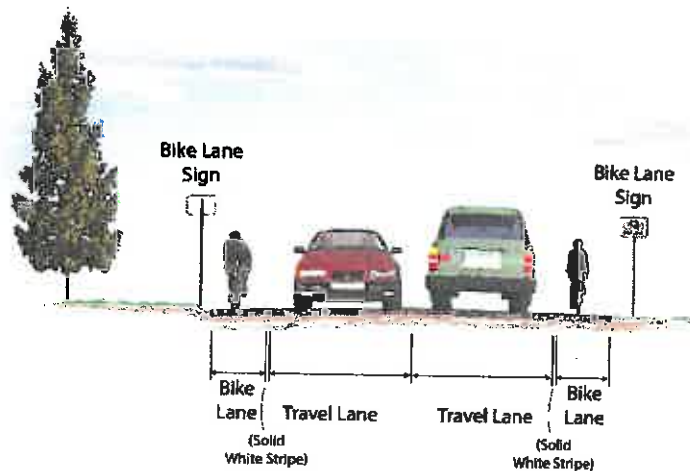


CLASS I - Multi-Use Path
 Provides a completely separated right-of-way for exclusive use of bicycles and pedestrians with crossflow minimized.



MUTCD R44A (CA)

- + **Class II Bike Lane.** Provides a delineated right-of-way assigned to bicyclists to enable more predictable movements, accommodating bicyclists through corridors where insufficient room exists for side-by-side sharing of existing streets by motorists and bicyclists.

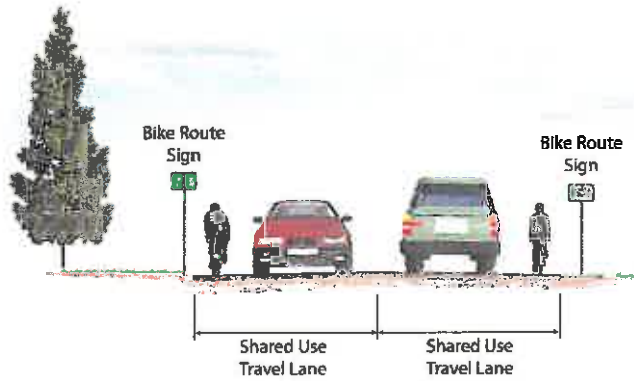


CLASS II - Bike Lane
 Provides a striped lane for one-way bike travel on a street or highway.



MUTCD R81 (CA)

- + **Class III Bike Route.** Shared facility that serves either continuity to other bicycle facilities or designates preferred routes through high demand corridors.



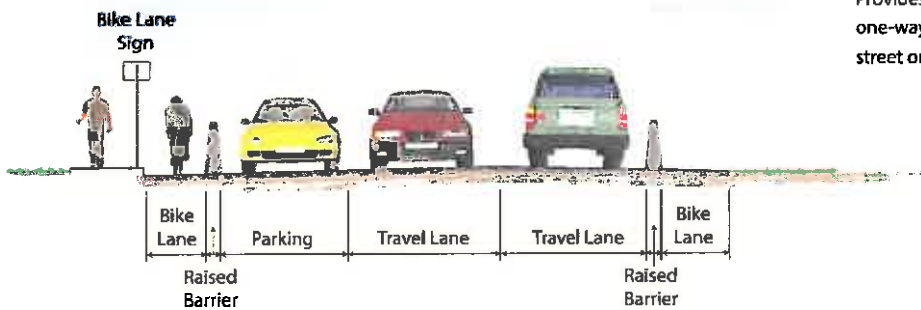
CLASS III - Bike Route

Provides a shared use with pedestrians or motor vehicle traffic, typically on lower volume roadways.



MUTCD D11-1

+ **Class IV Separated Bikeway or Cycle Track.** Provides delineated right-of-way assigned to bicyclists that have a physical separation between them and a vehicle. This separation can include parked vehicles, bollards, curbs, or any other physical device that provides this separation. This “new” bicycle classification was defined in AB 1193, amended the streets and highways code to allow for this treatment, and directed Caltrans to update Chapter 1000 of the Highway Design Manual to include this facility (which is currently being updated). One example of a future Class IV facility is a CV Link spur route on Avenue 48 which will be a pilot project for the City.



CLASS IV - Separated Bikeway (Cycle Track)

Provides a protected lane for one-way bike travel on a street or highway.

In addition to the four facility types described above, low traffic volume local streets also contribute to the bicycle network. These streets typically do not have a bicycle lane or signage; however, as a result of low traffic volume, it may be adequately safe and efficient as bike routes.

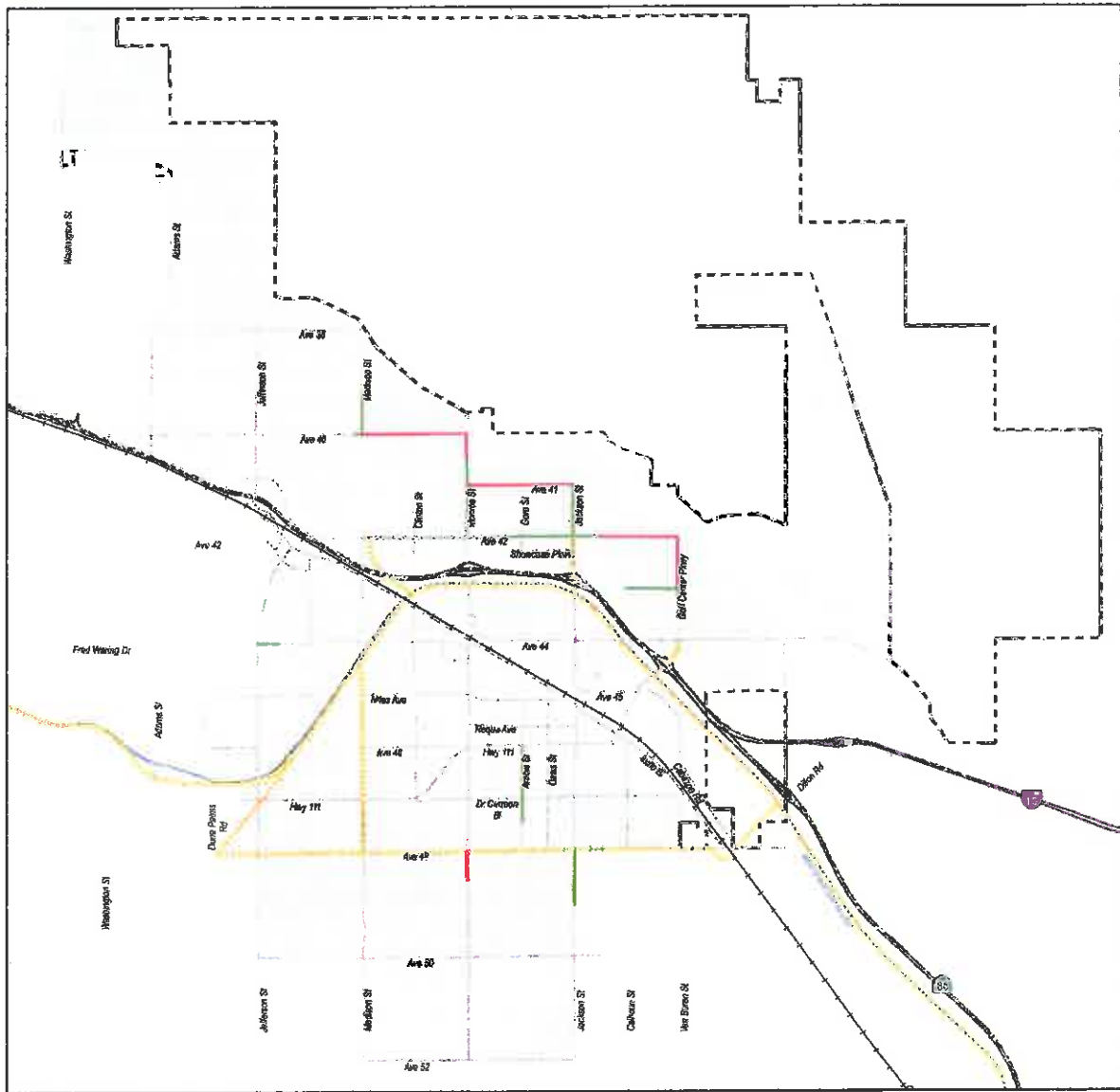
Golf Cart Network

A golf cart network can provide enhanced mobility for short trips in Indio. It is also an environmentally-superior travel mode compared to the automobile as most carts are electric and produce fewer emissions. Golf carts are also smaller, reducing the wear on City facilities and reducing the needed parking footprint, and they can provide a fun mobility option for persons who are not able to walk or bike for short distance trips. As such, the golf cart network in Indio can play a key mobility role for the City.

The City will continue to promote and support use of golf carts through the maintenance of the existing facilities and support of future facilities that are planned in the City. Figure 4-5 presents the existing and proposed golf cart routes within the City.

The Coachella Valley has been a leader in providing golf cart pathways as part of the off-street and on-street system. The CV Link project will provide golf cart accessibility for the entire region, connecting most of the cities and providing an off-street facility for longer distance golf cart trips.

Figure 4-5: Existing and Planned Golf Cart Routes



Existing and Planned Golf Cart Routes



- City Limits
- Sphere of Influence
- Roads
- Railroads
- Highways
- Bike Lane
- Sidewalk
- Future Sidewalk
- Planned CV Link and Spurs

Source: Riverside County/LARCIS (City Boundary 05/12, 05/14/2017); Riverside County/TDMA (Roads/Railroads/Highways); Riverside County (River)

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Transit

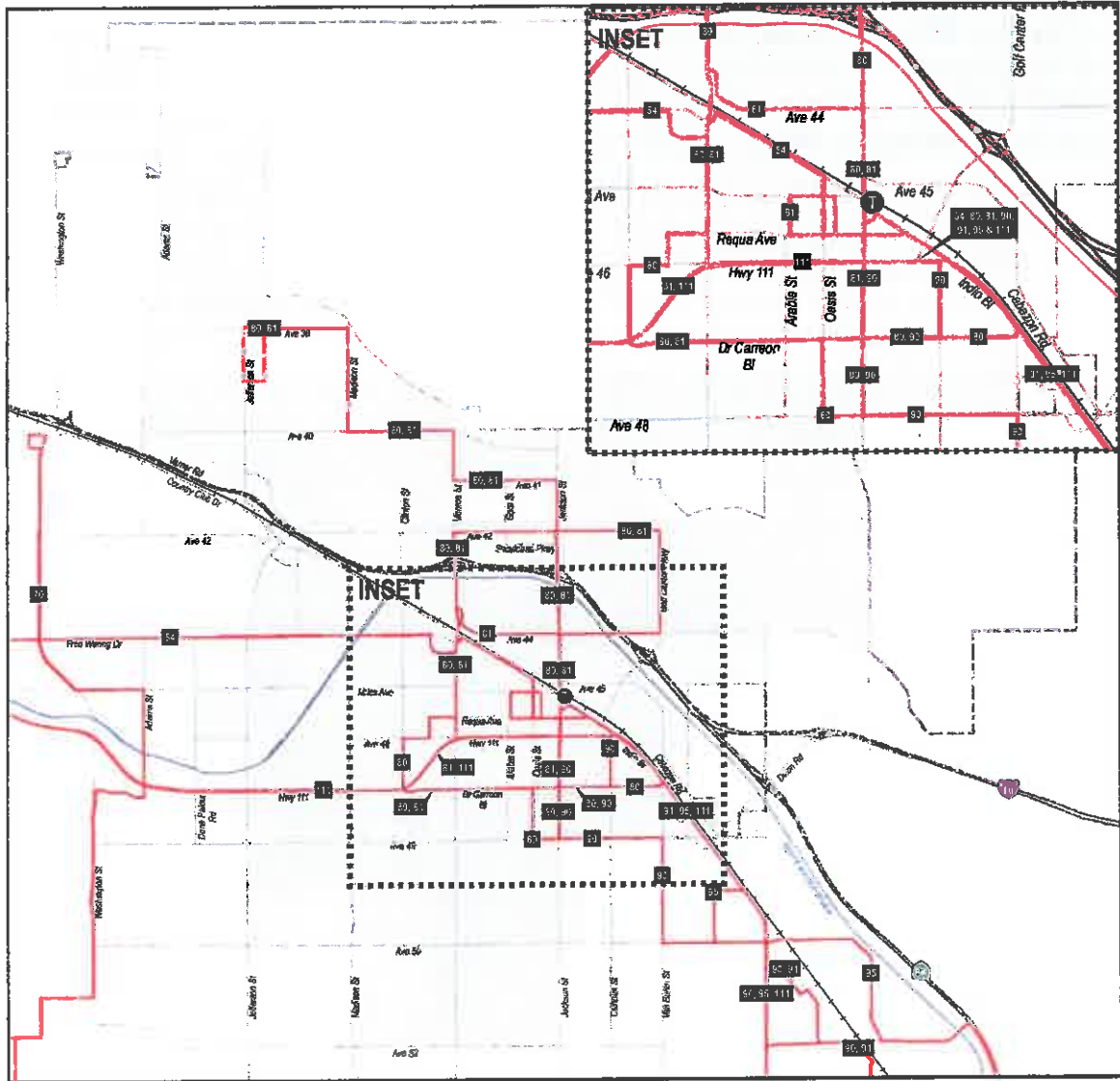
Transit typically consists of publicly-operated services, such as buses, trains, paratransit, commuter rail, and streetcar services. Currently, SunLine Transit operates a variety of bus routes in Indio. SunLine transit provides good access to activity centers in the City, particularly around Downtown and Highway 111. This mobility element seeks to enhance accessibility to transit to provide more facilities and connecting services to residents and visitors.

As shown on Figure 4-6, the City is working with other regional planning agencies to create opportunities for future rail options for the City. The goal of rail options is to ultimately serve the residents and visitors by providing a commuter rail connection to and from the City of Riverside and other part of southern California. As such, the element provides policies that support this desire, including the identification of a potential transit station near the City's Downtown.

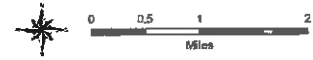


Public transit in Indio.

Figure 4-6: Proposed Transit Network



Proposed Public Transit Network



- City Limits
- Sphere of Influence
- Roads
- Railroads
- Highways
- SunLine Transit Routes
- Realigned Route
- Planned Transit Center

Source: Riverside County APCO / City Boundary 2012; SOI 2012; Riverside County TMA; Roads, Railroads, Highways; Riverside County (Rivers)

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Vehicle Circulation

To provide efficient vehicular circulation in the City, this Mobility Element provides policies related to vehicle mobility, which is a major mode of transportation for residents and visitors of the City. Figure 4-7 provides the City's recommended priority roadway network and Figure 4-8 presents the City's recommended street typologies.

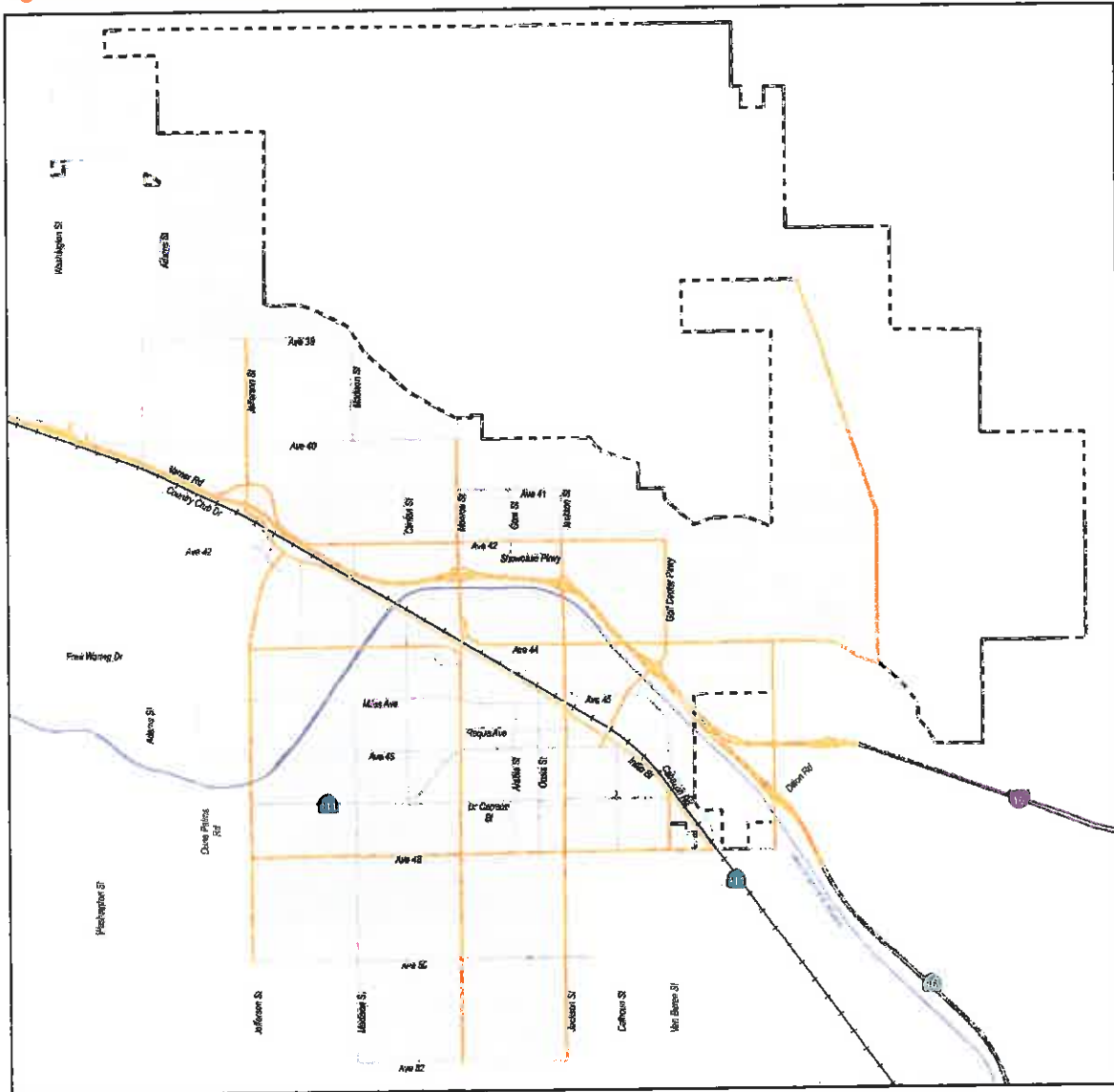
Although the State and the region have been deemphasizing auto travel as a primary mode of travel due to environmental impacts associated with auto travel (e.g. vehicle emissions), most residents in the City value use of their automobile and using vehicles in Indio is still a priority. The City will continue to use vehicle levels of service (LOS) as a key metric to measure the freedom to maneuver vehicles. Definitions for LOS are summarized below and are based on information from the Transportation Research Board's *Highway Capacity Manual*. Please note that LOS E represents "at capacity" operations.

Table 4-1 Level of Service

Auto Level of Service Definitions	
LOS	Characteristics
A	Primarily free-flow operation. Vehicles are completely unimpeded in their ability to maneuver within the traffic stream. Controlled delay at the boundary intersections is minimal. The travel speed exceeds 85% of the base free-flow speed.
B	Reasonably unimpeded operation. The ability to maneuver within the traffic stream is only slightly restricted and control delay at the boundary intersections is not significant. The travel speed is between 67% and 85% of the base free-flow speed.
C	Stable operation. The ability to maneuver and change lanes at mid-segment locations may be more restricted than at LOS B. Longer queues at the boundary intersections may contribute to lower travel speeds. The travel speed is between 50% and 67% of the base free-flow speed.
D	Less stable condition in which small increases in flow may cause substantial increases in delay and decreases in travel speed. This operation may be due to adverse signal progression, high volume, or inappropriate signal timing at the boundary intersections. The travel speed is between 40% and 50% of the base free-flow speed.
E	Unstable operation and significant delay. Such operations may be due to some combination of adverse signal progression, high volume, and inappropriate signal timing at the boundary intersections. The travel speed is between 30% and 40% of the base free-flow speed.
F	Flow at extremely low speed. Congestion is likely occurring at the boundary intersections, as indicated by high delay and extensive queuing. The travel speed is 30% or less of the base free-flow speed. Also, LOS F is assigned to the subject direction of travel if the through movement at one or more boundary intersections have a volume-to-capacity ratio greater than 1.0.

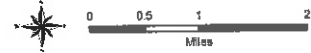
Source: Highway Capacity Manual 2010.

Figure 4-7: Auto Priority Streets



Automobile Priority Streets

-  City Limits
-  Sphere of Influence
-  Roads
-  Railroads
-  Highways
-  Auto Priority



Source: Riverside County/ARC0; City Boundaries 2012; 804/2012; Riverside County/TDMA (Roads/Railroads/Highways) Riverside County (Rivers)

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Another metric used is called Vehicle Miles of Travel (VMT). VMT is a metric of land use efficiency. A lower VMT reflects either lower trips generated by a land use or shorter trip lengths associated with vehicle trips. Additionally, new State guidance is under the rule making process that will require California Environmental Quality Act (CEQA) documents to utilize VMT as the transportation metric for identifying transportation impacts (VMT is replacing LOS in this respect). Although freedom to maneuver is important to residents, VMT will be required for future CEQA documents.

In addition to these performance metrics, this Mobility Element provides guidance related to Street Typology. Street Typology differs from Roadway Classifications in that it identifies the character and purpose of the street, recognizing the context of the street related to the prioritized user of the street and the interaction of the street with adjacent land uses. Table 4-2 describes the street typologies and figure 4-8 illustrates those typologies.

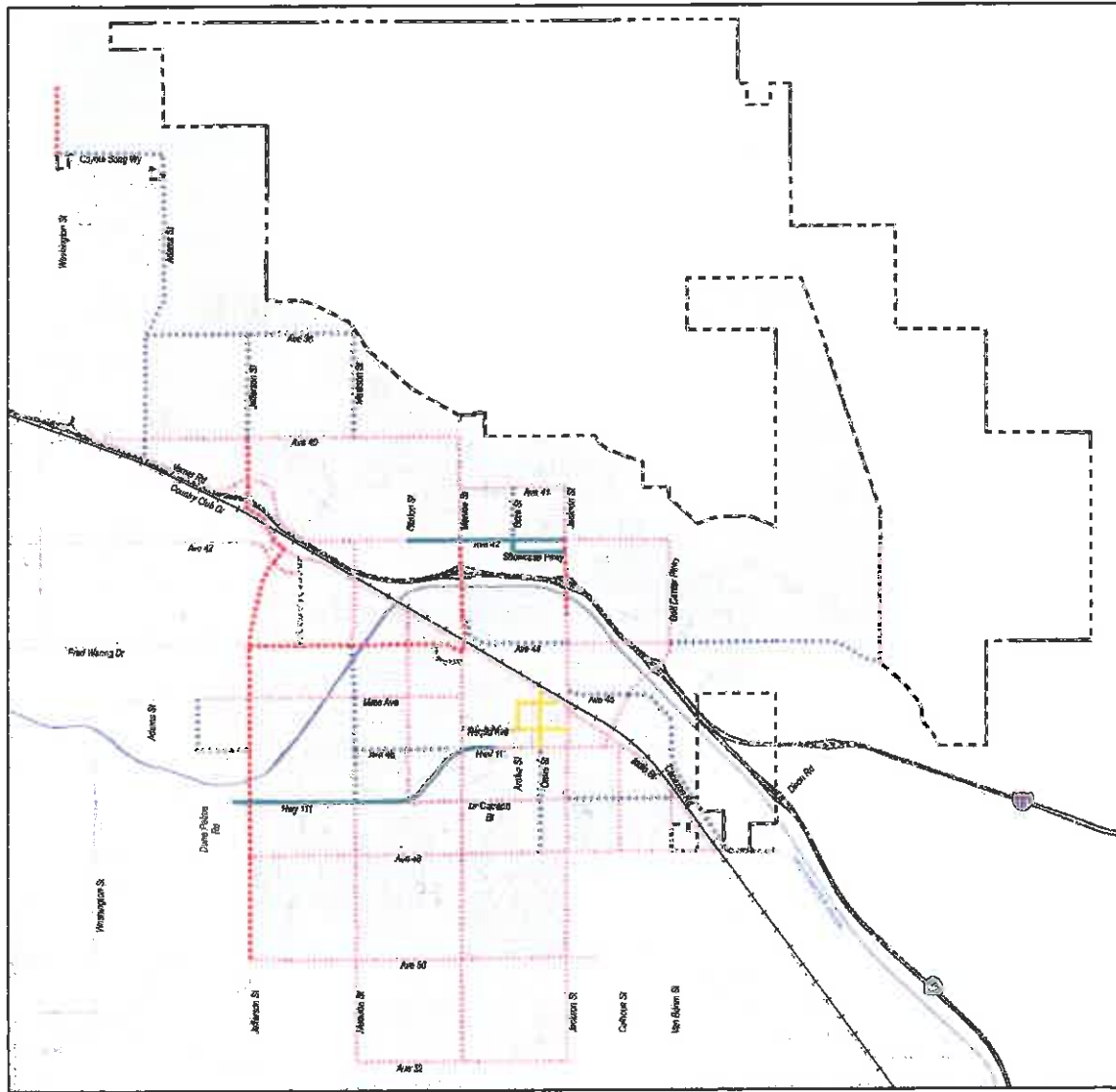
Table 4-2: Street Typology and Priority Modes Matrix

Street Typology and Priority Modes	Street Typology Attributes
<p>Freeways <i>Priority Autos, Buses</i></p>	<ul style="list-style-type: none"> • High-speed facilities designed to accommodate vehicles moving through the City • Bicycles and pedestrians are prohibited
<p>Arterial <i>Priority: Autos, Trucks, Buses</i></p>	<ul style="list-style-type: none"> • Arterials should provide four to six travel lanes. • Vehicular efficiency shall be prioritized. • Traffic signals shall be coordinated to prioritize vehicle movements. • Bicycle lanes can be provided and can be further enhanced or complemented by other facilities or off-street pathways. • Pedestrian facilities should be provided consistent with ADA requirements. • Mid-block crossings should not be provided. • Parking should be prohibited along these corridors. • Traffic calming techniques should not be considered.

<p>Boulevard</p> <p><i>Priority Autos, Bikes, Pedestrians</i></p>	<ul style="list-style-type: none"> • Boulevards should have two to four travel lanes and a median, as shown on Figures 4-9 – 4-11 • Bicycle lanes should be provided • Vehicle speeds should complement the adjacent land uses, typically 35 MPH or less • Off-street bicycle parking should be provided in retail areas • Bike racks may be provided within the public right-of-way and encouraged on private property • Traffic calming devices, such as curb extensions (bulbouts) or enhanced pedestrian crossing may be implemented • Street furniture shall be oriented toward the businesses • Mid-block pedestrian crossings could be provided at appropriate locations (e.g. where sight distance is adequate and speeds are appropriate) • On-street vehicle parking should be provided. In areas with high parking demand, innovative parking management techniques should be implemented / considered. • Pedestrians should be “buffered” from vehicle traffic using landscaping or parked vehicles.
<p>Collector</p> <p><i>Priority: Pedestrians, Bicycles, Autos</i></p>	<ul style="list-style-type: none"> • Collector should provide either two lanes without a median or two lanes with a median or center left-turn lane, as shown on Figures 4-12 and 4-13. • The primary function of the street is to connect people and different areas and land uses of the City to each-other directly or by connecting to/from arterial streets. • Collectors should provide for space vehicles, bicycles, and pedestrians. • Bicycle lanes should be provided. Bicycle Boulevards may be considered. • Pedestrians should be accommodated on sidewalks adjacent to the travel way (minimum 5’ wide sidewalk). • Mid-block pedestrian crossings and traffic calming devices may be provided, but it should only be considered at locations in areas with high-activity levels or destination attractions. • Parking may be provided on-street.

<p>Downtown Street</p> <p><i>Priority Pedestrians, Bicycles</i></p>	<ul style="list-style-type: none"> • Streets should provide two travel lanes with left turn lanes but can be four travel lanes (although not preferred) • Pedestrians shall not cross more than five travel lanes • Vehicle speeds shall be managed to support adjacent land uses (typically less than 30 MPH) • Enhanced bicycle and pedestrian crossings should be provided, including <ul style="list-style-type: none"> ○ Enhanced pedestrian notifications (e.g. responsive push-button devices) and treatments to enhance pedestrian visibility, and ○ Enhanced bicycle detection • Bicycle lanes shall be provided and can be further enhanced or complimented by other facilities or off-street pathways • Pedestrian sidewalks should be a minimum of five feet and shall strive for six to eight feet in width and shall conform to ADA requirements • Wider sidewalks may be provided in high pedestrian activity areas or where on-street dining is provided • Opportunities for mid-block pedestrian crossings should be investigated • Traffic calming devices that improve service levels for prioritized modes should be considered • On-Street parking is typically provided in a variety of forms, including parallel, angled, and back-in angled parking configurations
<p>Other Pedestrian Priority Areas</p> <p><i>Priority: Pedestrians, Bicycles</i></p>	<ul style="list-style-type: none"> • No more than four vehicle travel lanes are typically provided. • Vehicle speeds shall be managed to support adjacent land uses (typically 30 MPH or less). • Enhanced bicycle and pedestrian crossings should be provided, including: <ul style="list-style-type: none"> ○ Enhanced pedestrian notifications (e.g. responsive push-button devices) and treatments to enhance pedestrian visibility; and ○ Enhanced bicycle detection. • Bicycle lanes shall be provided and can be further enhanced or complimented by other facilities or off-street pathways. • Pedestrian facilities should be a minimum of five feet and shall strive for six to eight feet in width and shall conform to ADA requirements. • Wider sidewalks may be provided in high pedestrian-activity areas or where on-street dining is provided. • Opportunities for mid-block pedestrian crossings should be investigated. • Traffic calming devices that improve service levels for prioritized modes should be considered. • Parallel parking is typically provided.
<p>Local / Neighborhood Street</p> <p><i>Priority: Pedestrians, Bicycles</i></p>	<ul style="list-style-type: none"> • Local or neighborhood streets provides access to residential use as shown on Figures 4-14 and 4-15 • Pedestrians should be accommodated on a sidewalk • Bicycles may be accommodated with a bicycle route (Class III) if vehicle volumes and/or speeds necessitate, otherwise, bicycles can share the roadway • Bicycle boulevards may be considered • Traffic calming measures should be supported • On-street parking should be provided • Pedestrian facilities should be provided consistent with ADA requirements

Figure 4-8: Street Typologies



Street Typologies



- City Limits
- Sphere of Influence
- Railroads
- Highways
- Arterial
- Boulevard
- Connector with Median
- Connector without Median
- Downtown Streets
- Pedestrian Priority
- Local Streets

Source: Riverside County/LAFCD City Boundary 2012, 6/01/2012; Riverside County, T.M.A. (Roads/Railroads/Highways) Riverside County (Shaw)

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Figure 4-9: Street Cross Section Example: Boulevard with 2 Travel Lanes and Bicycle Lanes



Example implementation location: Dr Carreon

Figure 4-10: Street Cross Section Example: Boulevard with 4 Travel Lanes



Example implementation location: Monroe Street north of Highway 111

Figure 4-11: Street Cross Section Example: Boulevard with 4 Travel Lanes and Bicycle Lanes



Figure 4-12: Street Cross Section Example: Connector Street



Example implementation location: Avenue 48th East of Dillon Road

Figure 4-13: Street Cross Section Example: Connector Street with Class II Bike Lanes



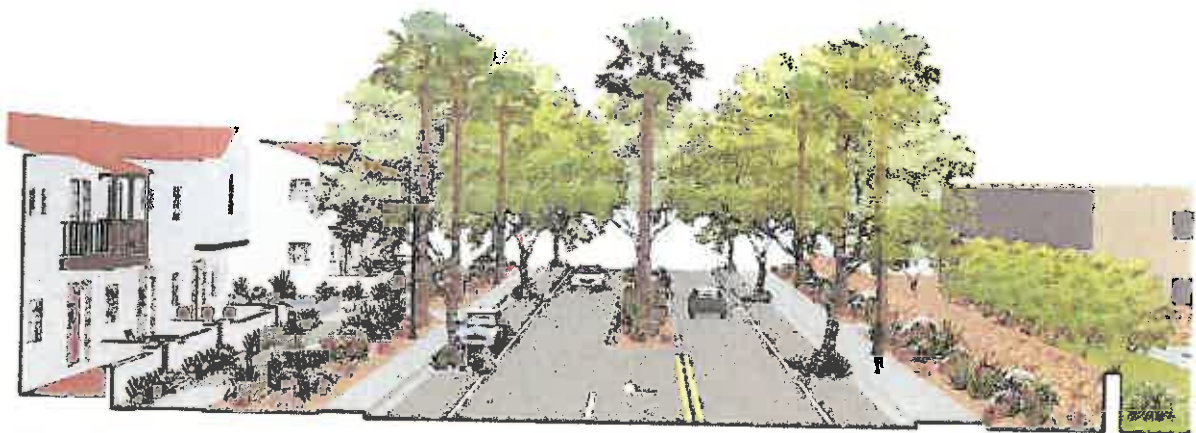
Example implementation location: Requa Avenue east of Monroe Street

Figure 4-14: Street Cross Section Example: Neighborhood / Local Street



Example implementation location: John Nobles Aveune east of Monroe Street

Figure 4-15: Street Cross Section Example: Neighborhood / Local Street



Example implementation location: Rubidoux Street south of Highway 111

Goods Movement and Airport Compatibility

Facilities that provide for the movement of goods and services in the City consist of the Union Pacific rail corridor that traverses east and west through the City and designated truck routes. These facilities are shown on Figure 4-16.

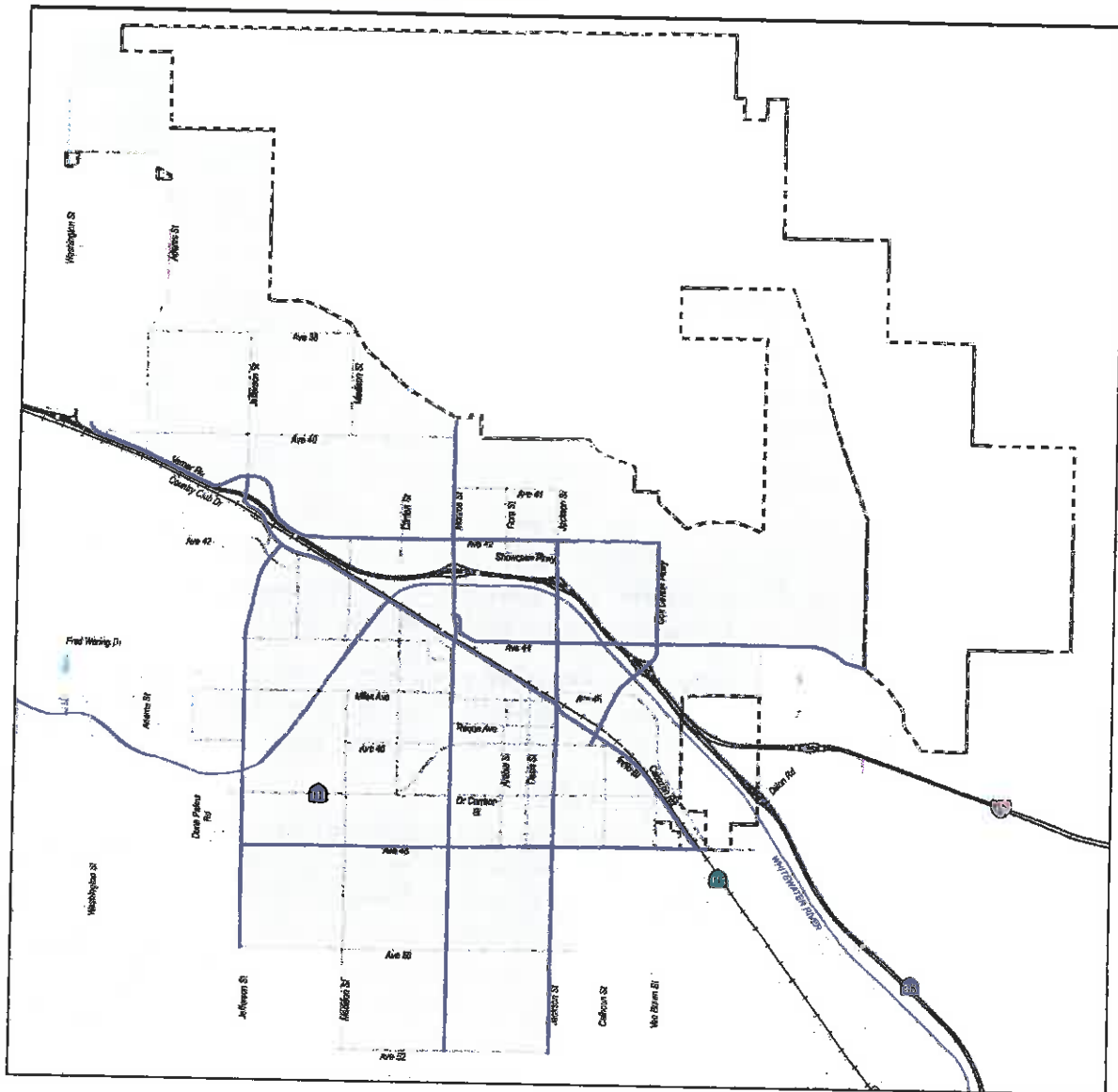
Many of the existing truck routes overlap with the vehicle and bicycle priority routes identified in the layered streets approach. This is not desirable as trucks and bicycles should be prioritized on different routes to the extent possible. As such, this Mobility Element recommends an update to the City's current truck route map to reflect the changes in the City.

In addition to the goods movement corridors noted above, the Bermuda Dunes Airport is adjacent to the City of Indio. As such, the airport flight approaches and departures does influence potential land use within the City to ensure compatibility with the airport and its flight paths. Most of the compatibility requirements related to types of use and building heights within flight paths of the airport. This mobility element ensures that development near the airport remains consistent with the Bermuda Dunes Airport Master Plan.



Goods movement through Indio

Figure 4-16: Potential Truck Priority Streets



Potential Truck Route Priority Streets



- City Limits
- Sphere of Influence
- Roads
- Railroads
- Highways
- Truck Routes

Source: Riverside County/LAFDD (City Boundary,2012; 601,2012)
 Riverside County/TDMA (Roads, Railroads, Highways) Riverside County
 (2014)
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 contributors, sourced above. Care was taken in the creation of this map
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Parking

Parking is critical to the success of both residential neighborhoods and commercial businesses. Inadequate parking means that businesses and residents find it difficult to access their destination. For example, visitors to commercial centers may “spillover” into surrounding neighborhoods. On the other hand, too much parking underutilizes valuable land, resulting in lower-density development that discourages other forms of transportation, such as walking and biking. Providing too much parking may also spread out land uses, creating gaps between businesses and store fronts, increasing vehicular use, and introducing additional points of conflict between pedestrians and vehicles. Too much, or too little, parking may also impact Downtown and Midtown redevelopment. Therefore, it is crucial to “right size” and manage parking such that the required spaces are balanced with the number of space needed.

The City’s Municipal Code provides standards for parking facilities based on land use type within the City. The Code also identifies bicycle parking requirements and allows parking reductions due to transit proximity and shared parking between land uses.

The City will continue to promote “right sizing” of parking facilities, by applying the following techniques:

- ✦ **Park once:** A strategy in destination centers to enable visitors to “park once” and visit a series of destinations on foot. Park-once strategies will work well in areas, such as Downtown, Midtown, and other pedestrian-priority areas that are well-connected by pedestrian and bicycle facilities. The creation of well-signed centralized parking areas supports this strategy.
- ✦ **Shared parking:** The City will continue to allow land uses that have different parking demands at different times of the day to share the same parking facilities. This is an effective way to minimize paved area, allow denser land use, provide for more landscaping and open space, and provide improved walkability within a mixed-use area.
- ✦ **Unbundled parking.** Rather than provide free guaranteed parking, “unbundling” parking from a development project requires residents and/or employees to separately pay for use of a parking space. Unbundling parking will allow tenants to choose whether or not to purchase (or rent) a parking space, reducing rental or ownership costs and making housing and commercial space more affordable. It may also reduce the total amount of parking required for the building lowering construction costs.
- ✦ **In-lieu parking fees.** Allowing developers to contribute fees toward the development of a common parking facility in lieu of providing on-site parking is an important tool to creating shared, park-once locations. This strategy will work best in centers, such as Downtown and Midtown, assisting in paying for structured parking and providing developers an opportunity to increase density on their parcels.
- ✦ **Parking management strategies.** The City may also manage high-demand parking locations and destinations through a number of different strategies, including demand pricing, time restrictions, valet parking, and other techniques.
- ✦ **Public-private partnerships.** The City, business owners, and developers collaborate to provide both private and public parking opportunities. Instances where this works well include parcels

owned by the City, or private underutilized parking lots where a private entity develops, manages, and enforces parking in these public lots.

- + **Parking locator signs.** Electronic monitoring devices that identify the available parking in a given facility and utilize changeable message signs to assist travelers in identifying available parking locations. This may require modifications to the City's Zoning Ordinance to be implemented in some areas of the city.
- + **Reduced parking standards.** Reducing parking standards in Downtown, Midtown, and other areas that are well-served by transit and providing parking cash out programs (employers pay employees to not drive a single occupancy vehicle to work) lower parking requirements and help to right size onsite parking.
- + **Biking equals business program.** Businesses may provide bicycle parking or corrals and may provide incentives to encourage their patrons and employees to ride rather than drive. For certain businesses, the City may reduce required onsite parking for vehicles if they provide a bicycle corral that accommodates more people.
- + **Transit equals business program.** Businesses may provide their customers and employees incentives to encourage them to use transit rather than drive.

Although there are additional parking strategies that are available and may become available in the future, most of the strategies work best in walkable, mixed-use areas, including the Connected Neighborhoods, Neighborhood Centers, and Downtown, as identified in the Land Use and Community Design Element.



Commercial center parking

Transportation Network Companies and Autonomous Vehicles

The future of mobility is changing rapidly. Transportation network companies (TNCs), such as Uber and Lyft, are increasing in popularity and usage. TNCs combine web-based applications, such as mobile applications, and ridesharing vehicles. While TNCs could reduce the number of vehicles on the roadway network, it is also likely to increase VMT as transportation becomes more convenient, especially for the young and elderly. Studies suggest that the limited amount of curb space requires public agencies to make clear choices and tradeoffs about the priority of curb space if they desire efficient operations.

There is a growing trend linking TNCs and transit. TNCs often provide transportation to and from key destinations, such as transit stations. The growth in use of TNCs will be dependent on the ability to expand coverage, reach new passengers while retaining existing passengers, attract drivers, work with cities and regulators, show that TNCs can have a profitable business model, and to develop autonomous technology that will fold into a shared AV fleet network. This Mobility Element guides the policy directives to embrace TNCs and provide infrastructure to support their increase in usage. Policies also address curbside management, such as areas that either prioritize or restrict passenger loading and unloading.

For AVs, there are several levels of vehicle autonomy. They range from cruise control (low level of autonomy) to fully-autonomous vehicles (AVs) that require no interaction with the driver. Manufacturers are developing this technology, and the federal and state governments will likely determine regulations for a fully-autonomous fleet.

It is unknown how long the vehicle fleet will take to turn over to a fully autonomous fleet. However, it is being developed and is something that this element recognizes from a policy perspective.

This expansion will likely change future travel behaviors in the City. AVs are expected to make car travel less stressful, increase travel safety, and reduce operational inefficiencies on freeways. AV interactions with pedestrians and cyclists will also require careful planning and design. In a shared AV fleet, the need for that many parking spaces will not be required and this element directs the City to adjust parking requirements accordingly. This Mobility Element provides guidance to embrace a future AV fleet and provide infrastructure to support this fleet.

Transportation Management

This Mobility Element also supports and promotes Transportation Demand Management (TDM) and Transportation Systems Management (TSM) techniques. TDM consists of programs and policies to reduce the demand for the single-occupant automobile. Common techniques include carpool programs, car-sharing and bike-sharing programs, flexible work hours, telecommute provisions, shuttle services to nearby transit stations, employee transit subsidies (e.g. employers will subsidize bus or rail tickets), installation of bicycle facilities (lockers, racks, lanes, showers at employment areas, etc.), or other measures that would reduce the demand to drive. TDM is critical for the City to build-out without over expanding infrastructure and serving the “first mile/last mile” component of a transit trip.

The City’s future transit effectiveness will be dependent on major employers assisting with providing TDM measures. TDM is envisioned to include potential shuttle circulators from transit stations to major employers and destinations, showers and changing rooms at those locations, and a host of other typical

TDM techniques that would support transit usage and the connection to the ultimate destination. This Mobility Element also supports TDM through potential incentives (such as reduced parking standards for TDM implementation) to further support transit access to these destinations¹.

The City has also implemented a state-of-the-practice Transportation System Management (TSM) system. This system integrates traffic signals in the City to a single access point, allowing City staff to monitor and update signal timings to improve safety and mobility for all users in the City. This Mobility Element supports further implementation of this program and use of other technologies that become available, which have the ability to improve mobility for all users of the City's transportation system. TSM measures include, but are not limited to: improving design standards (e.g. adjusting design standards to reduce pedestrian crossing distances to promote pedestrian use while allocating more green time toward high demand vehicle movements), upgrading and coordination of traffic control devices, intelligent transportation systems (ITS) (e.g. "smart" systems that manage the system better, such as signal coordination, driver information signs, or other technologies that allow more efficient use of the transportation system), controlling on-street parking, and use of sophisticated electronic control methods to supervise the flow of traffic.



Traffic flow in Indio

¹ NCHRP Report 548-A

Goals and Policies

Complete Streets

Establishing a comprehensive complete streets policy requires ten key elements be addressed. These policies and guidance are provided below along with other policies supporting the implementation of complete streets.

Goal ME-1: Complete Streets. A City that embraces complete streets by providing streets that are safe and accessible by users of all ages and all abilities.

ME-1 Policies

ME-1.1 Vision. Utilize the layered networks approach to provide enhanced mobility for prioritized modes along streets as shown on Figure 4-1. This will guide investment along streets in the City.

ME-1.2 Users. Design and build streets that accommodate users of all ages and all abilities. This includes utilizing the layered networks approach noted on Figure 4-1 to identify key modes that shall be prioritized and enhanced along streets.

ME-1.3 Projects and Phases. Design, plan, maintain, and operate streets using complete streets principles for all types of transportation projects including design, planning, construction, maintenance, and operations of new and existing streets and facilities. This includes repurposing unneeded roadway pavement to implement bicycle and pedestrian improvements (e.g. road diets) when Average Daily Traffic (ADT) volumes are less than 20,000 vehicles.

ME-1.4 Street Connectivity. Encourage short block spacing for new development consistent with the Land Use and Community Design Element to enhance connectivity to neighborhoods. In key areas of the City (e.g. the pedestrian-priority areas, Downtown, Midtown, and the Festival District), work with existing land owners to improve connectivity for bicycles and pedestrians.

ME-1.5 Responsibility. All departments are responsible for implementing complete streets and incorporating these complete streets policies into their action plans and other planning documents.

ME-1.6 Design Criteria. Create engineering design and guidelines to specifically address the complete streets goals. This may result in updating the City's Engineering Standards and other City policy or ordinances.

ME-1.7 Context. Ensure that complete streets applications integrate the neighborhood and community identity into the street design. This includes recognition of the prioritized users of the street, as noted on Figures 4-1 – 4-6, 4-8, and providing enhanced treatments for those users, such as special provisions for bicycle and pedestrians on bicycle and pedestrian-prioritized streets.

ME-1.8 Performance Standards. Monitor and evaluate multi-modal performance standards, such as Multi Modal Levels of Service (MMLOS), as a means to measure the service levels of prioritized modes based on the layered networks approach. When and if these methodologies are applied in the City, LOS D or better for prioritized modes and LOS E or better for non-prioritized travel modes will be maintained unless exempted from this requirement (see implementation policy PWD-2). Where modes conflict, e.g. when a bicycle-priority street intersects with an auto-priority street, the most vulnerable user shall dictate modal priority.

- ME-1.9 Funding.** Pursue funding for multi-modal infrastructure projects that promote complete streets, such as impact fees and local, regional, State, and federal grants.
- ME-1.10 Future Improvements.** Obtain and preserve adequate right-of-way to accommodate future mobility system improvements.
- ME-1.11 Residential Streets.** Design residential streets to minimize traffic volumes and/or speed, as appropriate, without compromising connectivity for emergency first responders, bicycles, and pedestrians. This could be accomplished through management and implementation of complete streets strategies, short block lengths, narrow streets, and/or traffic calming measures.
- ME-1.12 Traffic Calming Tools.** Use traffic-calming tools to assist in implementing complete streets principles. Traffic calming tools include roundabouts, curb extensions, high-visibility crosswalks, and separated bicycle infrastructure.

Active Transportation

Establishing a comprehensive active transportation network requires more than implementation of a comprehensive complete streets policy. The following policies provide additional guidance related to active transportation systems.

Goal ME-2: Active Transportation. A City that provides a first-rate network of bicycle and pedestrian infrastructure.

ME-2 Policies

- ME-2.1 CV Link.** Support and complement implementation of the CV Link project by providing connections from CV Link to activity centers of the City and assisting in the thoughtful implementation of the facility. This includes CV Link connectivity, including CV Link spur routes, the Avenue 48 Class IV pilot project, and the North Spur Bike Path.
- ME-2.2 Pedestrian Priority Areas.** Monitor and work to transition areas shown on Figure 4-2 to a more pedestrian-friendly environment in the future. This would include deemphasizing these corridors as vehicle thoroughfares and creating better pedestrian environments with fewer travel lanes, slower vehicle speeds, and buffers between the road and the pedestrian area.
- ME-2.3 Facility Enhancement.** Enhance the bike and pedestrian-preferred facilities as identified in Figure 4-1 as part of development, private grants, signing of shared routes, maintenance activities, etc. The City will also complete and continually update a Complete Streets Master Plan which will also assist in enhancing bicycle and pedestrian infrastructure.
- ME-2.4 Safe Routes to School.** Encourage the use of bicycles and walking as the preferred transportation mode for trips to and from elementary, middle, and high schools by implementing a "Safe Route to School" Program and a bike safety education program.
- ME-2.5 Intersection and Signal Enhancements.** Enhance pedestrian and bicycle crossing efficiency and safety, including timing of signals, crosswalks, and intersection design features. Provide signal timing that allows intersection crossing according to California MUTCD guidelines.
- ME-2.6 Golf Cart Facilities.** Maintain and expand the golf cart network shown on Figure 4-4 to support that mode of travel within the City.

Transit

Indio strives to provide safe, affordable, and convenient public transportation for the community. The following policies provide additional guidance related to public transit.

Goal ME-3: Transit. The City will work with Sunline Transit and other regional partners to enhance bus transit, and to implement a future transit station in conjunction with the planned commuter rail extension to and from Riverside.

ME-3 Policies

ME-3.1 Riverside Commuter Transit. Support a potential expansion of commuter rail transit from Riverside to the Coachella Valley, including support for a transit station adjacent to the City's Downtown area.

ME-3.2 Local Service. Work with SunLine Transit to expand transit routes in the City and enhance bus stops in the City to provide shelters, secure bicycle parking, benches, and safe waiting areas at each stop.

ME-3.3 Safe Linkages. Encourage convenient and safe pedestrian linkages to and from transit service to provide better first-mile/last-mile connectivity. This includes connectivity to/from existing and new development and along streets providing access to the transit stop.

ME-3.4 Park-Ride Lots. Work to expand park-and-ride facilities by partnering with businesses that generate minimal parking demand during typical weekdays, such as assembly uses.

Vehicle Circulation

The following goals and policies relate to Indio's vehicle circulation.

Goal ME-4: Vehicle Circulation. The City will provide appropriate vehicle circulation, especially along streets identified as priority-auto streets.

ME-4 Policies

ME-4.1 Street Typologies. Minimize street widths to minimize capital costs, maintenance costs, decrease vehicle speeds, and improve safety for all users of the street while ensuring consistency with the Street Typology guidance provided in Table 4-1. This will require updates to the City's Engineering Standards and or the City's soon to be completed Complete Streets Master Plan

ME-4.2 Right of Way. Maintain appropriate right of way to implement the ultimate roadway sizing as shown on Figure 4-6.

ME-4.3 Highway 111. Monitor traffic volumes along Highway 111, and work to transition areas shown on Figure 4-2 to a more pedestrian-friendly environment in the future.

ME-4.4 Regional Coordination. Coordinate with Caltrans, CVAG, SCAG, and adjacent agencies to implement complete streets and maintain consistency with regional plans such as the Regional Transportation Plan and Sustainable Communities Strategy and Congestion Management Program.

Goal ME-5: Truck Routes and Airport Compatibility. The City shall maintain a safe and efficient system for delivering goods and services and ensure compatibility with the Bermuda Dunes Airport.

ME-5 Policies

ME-5.1 Rail. Support the continued use of the rail corridor through the City as a key goods movement corridor.

ME-5.2 Airport Compatibility. Support the continued use of the Bermuda Dunes Airport through ensuring consistency with the airport master plan.

Goal ME-6: TNCs and AVs. Proactively plan for and support changes in mobility technologies.

ME-6 Policies

ME-6.1 Adaptation. Monitor the effects of increased use of TNCs and AVs and adjust design standards and codes as needed to adapt to changing travel behaviors.

ME-6.2 Parking. New parking facilities will be planned to ensure a relevant use in the future if parking requirements are dramatically decreased due to new technologies.

ME-6.3 Curb Space Management. Manage curb spaces in activity areas to balance the demands of AVs and TNCs, bicycles, pedestrians, delivery loading/unloading, street furniture, etc. to ensure a balanced provision to all users.

Goal ME-7: Transportation Management. Utilize TDM and TSM techniques to maximize the City's current transportation infrastructure investments.

ME-7 Policies

ME-7.1 Transportation Demand Management. Utilize Transportation Demand Management (TDM) measures throughout the City, where appropriate, to discourage the single-occupant vehicle, particularly during the peak hours.

ME-7.2 Transportation System Management. Utilize Transportation System Management (TSM) measures throughout the City to ensure that the City's circulation system is as efficient and cost effective as possible and reflect the desire of the City to implement improved pedestrian and bicycle facilities.

ME-7.3 Festival Management. Work with event producers to control mobility during festivals. This includes appropriate circulation and connectivity for all modes of travel.

Parking

As parking needs continue to change, Indio will work to achieve parking ratios that serve the community while minimizing impacts on residents. The following goals and policies relate to parking within Indio.

Goal ME-8: Parking. Parking will be right sized within the City.

ME-8 Policies

ME-8.1 Off-Street Parking. Require new developments to provide sufficient off-street parking (or payment of in-lieu fees) to reduce on-street parking congestion and increase both auto and pedestrian safety. New development shall provide electric vehicle charging stations and preferential parking for carpools, vanpools, and alternative fuel vehicles.

ME-8.2 Off-Street Parking Alternatives. Allow developers to meet their minimum parking requirements via shared parking techniques that can leverage unused parking with nearby parcels, in-lieu fees, or on-street parking.

ME-8.3 Managed Parking Supply. Manage parking supply through implementation of time limits, pay parking, or permits, while ensuring the preservation of economic development goals.

ME-8.4 Bicycle Parking. Safe and secure bicycle parking facilities shall be provided with all new development.

Implementation Actions

The table below identifies programs, policy updates, planning efforts, coordination efforts, and other actions that will help implement the General Plan’s mobility vision and policies. Programs are consistent with this chapter’s goals and policies.

Table 4-3: Mobility Programs

	DESCRIPTION	PRIORITY	TIME FRAME	RESPONSIBILITY
1	Design Guidelines. Complete the City’s complete street design guidelines and update the City’s design standards for consistency with complete streets.	High	Short	Public Works
2	Protected Streets and Intersections List. Develop a list of protected streets and intersections that are exempt from a level of service standard (see policy ME-1.8) for any mode or that are exempt from implementing appropriate complete street standards. This list will be maintained by the City’s Public Works Department and shall be approved by City Council resolution. Conditions that would warrant a complete streets exception would include inadequate right-of-way, potential impacts to environmentally sensitive areas, the need to modify mode priority due to an adjacent land use compatibility issue, or the need to maintain enhanced designs for other prioritized modes.	High	Short	Public Works
3	Development Review. Update the City’s site plan review process and subdivision ordinance to require the provision of adequate bicycle and pedestrian access for new development projects.	High	Short	Planning, Public Works
4	SB 743 Monitoring. Monitor implementation of SB 743 and update the City’s CEQA guidelines to reflect revisions in CEQA requirements. The City’s consistency with SB 743 requirements does not relieve development from maintaining the City’s level of service requirements noted in Policy ME-1.8.	Medium	Short	Public Works
5	Truck Routes. Update the designated truck routes shown on Figure 4-8 to allow the safe and efficient movement of goods for commerce and industry, minimize conflicts with preferred modes (e.g. minimize truck routes along bicycle or pedestrian prioritized corridors), and minimize incompatibility with other sensitive land uses in the City including residential neighborhoods. The updated truck route map will be provided on the City’s website and will be adopted by City Resolution.	High	Short	Public Works
6	Curb-Space Allocation. Develop curb space priorities and guidelines to assist with actively managing increased demands on curb space utilization.	Medium	Medium	Public Works
7	Parking Requirements. Revise the Zoning Ordinance requirements for off-street parking to ensure consistency with the General Plan.	High	Short	Public Works, Planning
8	Dedicate City Staff for Complete Streets and Safe Routes to School Projects. Allocate additional staff resources to manage, implement and pursue funding for complete street projects, including the submittal of grant applications for bikeway, pedestrian and Safe Routes to School (SRTS) funding.	High	Ongoing	Public Works

9	Vehicle Idling. Establish a local ordinance that exceeds the state vehicle idling restrictions where appropriate, including restrictions for bus layovers, delivery vehicles, trucks at warehouses and distribution facilities and taxis, particularly when these activities take place close to sensitive land uses (schools, senior centers, medical facilities and residences).	Medium	Medium	Public Works, Planning
10	Collision Analysis. Analyze areas of high risk of pedestrian or cyclist danger and develop a priority list of future physical improvements that consider future land uses within the area.	High	Short	Public Works, Planning

A photograph of the exterior of Joe's Barber Shop. The building's facade is covered in a textured, tan-colored material with a repeating diamond or triangular pattern. The name "Joe's" is written in a large, blue, cursive font with a white outline. Below it, "BARBER" is written in a bold, red, sans-serif font, and "SHOP" is written in a bold, blue, sans-serif font. Three tall palm trees are visible against a clear blue sky with a few wispy clouds. The sun is visible in the upper right corner, creating a lens flare effect.

Joe's
BARBER
SHOP



chapter five

ECONOMIC DEVELOPMENT

The purpose of the Mobility element is to create a transportation network for the City that balances model priorities to address the safe and efficient operation, maintenance, and management of the circulation network. The goals and policies in the element have been developed to ensure that all streets within the City are reviewed through a "complete streets" lens – meaning that all streets should provide safe accommodation for all users of the transportation network. This approach does not mean that all streets should equally prioritize all modes of travel nor does it imply that one mode should dominate street planning on all streets within the City. Rather, different modes should be prioritized on different streets, providing a layered network of streets that responds to the context of the adjacent land use.

Key Considerations and Strategies

Indio has prepared an Economic Development Action Plan, which identifies strategies to strengthen and build upon the City’s key assets and opportunities for economic growth. The Action Plan contains the following community goals:

1. Bolster Indio’s tourism and hospitality sector
2. Strengthen Indio’s existing retail base and recruit new retail development
3. Create livable communities and facilitate neighborhood revitalization
4. Promote Indio’s image, identity, and opportunities
5. Be development ready
6. Seek alternative sources of sustainable revenue

The Economic Development Element of the General Plan builds upon this foundation, taking steps to foster continued economic growth and prosperity. This Economic Development Element will provide policies that support the community goals and vision that is stated by the General Plan as a whole. The Element is closely tied to several other elements that are associated with the physical development of Indio. It is correlated to the Land Use and Community Design Element in that the development pattern, mix of land uses, and aesthetic improvements are important components of a balanced and healthy economy. It is associated with the Housing Element, which addresses the provision of housing for all income groups to support a diverse population and labor force. It is related to the Mobility Element, which addresses the variety of transportation options available to access job centers, shopping areas, schools, and public facilities. It is also associated with the Conservation Element, which establishes a framework for the responsible and efficient use of resources. Additionally, the Economic Development Element is related to the Parks and Recreation Element insofar as parks, recreational facilities, and community gardens attract users and visitors and offer opportunities to host special events or support other economic development activities.

Figure 5-1: Economic Development and Planning



Festival City

As the “City of Festivals,” Indio is recognized as a premier location for arts and entertainment, education, commerce, history, and culture. The City annually hosts a variety of world-renowned cultural festivals and events, such as the Coachella Valley Music and Arts Festival and the Stagecoach Country Music Festival, that attract nearly 12 million visitors. Increasing the ability of the City to capture a higher proportion of these taxable expenditures is an important opportunity. To approach this opportunity, the City can focus on expanding the development of tourism-related uses, specifically in Midtown, the Fantasy Springs Leisure and Hospitality District, Empire Polo Club Resort and Entertainment District, and Polo Square Lifestyle Zone. The addition of lodging, dining, and hospitality businesses will encourage visitors to stay in Indio, which will create economic benefits for the entire community.

Economic Diversity

Indio is the original City of the Coachella Valley and for most of its history the community has served as an important center for regional commercial activity. With over 2,000 businesses ranging size from small businesses to large scale commercial enterprises, Indio is continually in the process of reinventing and growing as a vibrant community with a strong and diverse commercial base.

Downtown and Midtown

The revitalization of Downtown has been a priority for many years. While the recent development of the College of the Desert’s Eastern Valley Campus, Loma Linda Children’s Clinic, and the Coachella Valley Arts Center and History Museum has brought new life to Downtown, the necessary catalytic projects to modernize Downtown have not yet materialized. To address these concerns, the City has prepared a Downtown Specific Plan that seeks to transform the area into a vibrant, mixed-use destination. The California Desert Trial Academy recently opened, and the new East County Detention Center is currently under construction, reinforcing Indio’s role as the second county seat. To expand economic growth, the City is engaging in public-private partnerships with the Coachella Valley Economic Partnership to establish a business incubator that would provide assistance to startup companies. Such assistance may include low-cost office space and business support resources. Collectively, these projects could bring additional year-round activity to Downtown.

Similar to Downtown, the Midtown / Indio Fashion Mall District is in need of redevelopment. Located at the crossroads of Highway 111 and Monroe Street, this district has the potential to become a prime retail and entertainment destination. However, the abundance of vacant retail space and vacant and underutilized lots has hindered the revitalization of this area. The City of Indio has been working with property owners to revamp the area and attract regional visitors. Further retail and residential mixed-use development opportunities may emerge along Highway 111 as the Midtown area comes to evolve.

Land Uses

As Indio continues to grow and attract new businesses, developers have seen the City’s full, long-range potential as a commercial location. In recent years, major retail development has emerged along I-10 and Highway 111, with the development of power centers, an auto mall, and small-scale retailers. These new

businesses have not only increased retail and taxable sales opportunities but have also provided Indio with a variety of employment opportunities.

To expand economic growth, Indio has designed the Land Use and Community Design and Transportation Elements to accommodate compact, mixed-use environments that are linked together by an attractive network of pedestrian and bicycle friendly streetscapes. Providing such uses promotes economic growth by supporting local businesses, creating employment opportunities, and giving developers an incentive to invest in the City. Through this Economic Development Element, Indio continues to implement actions that reinforce the City's reputation as the place for opportunity and growth.

Workforce Development

To maintain a highly educated and skilled workforce, Indio is partnering with local school districts and appropriate agencies to provide job training programs that can prepare the local workforce for occupations in new and emerging industries. This increases the economic competitiveness of the local labor force, which fulfills Indio's economic development objective to become a key employment center for the Coachella Valley.



College of the Desert near Downtown Indio

Goals and Policies

A strong, diversified economy and tax base increases prosperity for both residents and businesses. With an increasingly educated and skilled workforce, it ensures that Indio secures its role as an employment center for the Coachella Valley with quality jobs and housing and is viewed as a desirable place to work, shop, and live. It also serves to retain existing and attract new businesses as well as promote long-term economic vitality. It further seeks to facilitate economic growth in both new, emerging areas of Indio and older areas in need of revitalization.

This section provides goals and policies that enhance the economic well-being of Indio. This Economic Development Element provides policies and goals on a city-wide basis with more focused recommendations integrated into the Land Use and Community Design Element by subarea within Indio. These policies are designed to facilitate business retention and expansion, job growth, an educated and skilled workforce, and an economically-viable community that supports a high-quality of life.

Goal ED-1: Vibrant Economy. A vibrant economy that maintains existing businesses and attracts new development.

ED-1 Policies

- ED-1.1 Regional and Local Partnerships.** Engage in public-private partnerships with local and regional organizations, such as the Coachella Valley Association of Governments (CVAG), the Indio, Coachella, La Quinta, Palm Desert, and Coachella Valley Hispanic Chambers of Commerce, the Coachella Valley Economic Partnership (CVEP), and the East Valley Coalition (EVC).
- ED-1.2 East Valley Coalition (EVC).** Support the EVC partnership between La Quinta, Coachella and Riverside County to jointly market and promote economic development in the Eastern Coachella Valley.
- ED-1.3 Permitting Process.** Improve the permitting process where businesses can identify their economic development requirements early and work with a dedicated staff member who will guide them along the way in order to ensure that development is realized in a timely manner.
- ED-1.4 Project Review.** Provide a timely and thorough review of proposed economic development projects.
- ED-1.5 Financing Priorities.** Set public infrastructure financing priorities by key economic focus areas as identified in the City's Economic Development Action Plan as they are updated and revised over the life of the General Plan
- ED-1.6 Business Outreach.** Expand the City of Indio's economic development role to include a regular outreach effort to better understand and respond to the ongoing economic needs of existing and new businesses in Indio.
- ED-1.7 Branding Program.** Maintain a unique brand comprised of visual themes and taglines that distinguish Indio from other jurisdictions in the Coachella Valley and accentuate its opportunities and attractions.

- ED-1.8 Marketing Program.** Support marketing programs that emphasize Indio's unique economic opportunities including, cultural, arts, and environmental advantages and attributes.
- ED-1.9 Placemaking.** Support a variety of revitalization and improvement programs focused on placemaking and beautification, such as facade improvements, small plazas, public art, and community events.
- ED-1.10 Flexible Spaces.** Promote flexible work spaces that can be shared among commercial and/or educational tenants to support entrepreneurship, affordability, sharing of resources and appeal to start-ups and/or spin-offs from regional institutions of higher learning and the modern workforce.

Goal ED-2: Employment Base. A growing employment base with a diversity of job opportunities for current and future residents.

ED-2 Policies

- ED-2.1 Business Development Opportunities.** Identify business development opportunities that will increase the number of professional and technical jobs as well as increase skill and wage levels. Emphasis should be placed on development opportunities that diversify the local economy.
- ED-2.2 Incubators.** Encourage development of incubator facilities in Indio through the provision of low cost space and business resources that support the startup and expansion of firms with creative new ideas and products; work cooperatively with the California and Coachella Valley Innovation Hub (iHub) Programs.
- ED-2.3 Infrastructure Improvements.** Identify public services and public infrastructure improvements necessary to expand existing and encourage new industrial development. Conduct regular business surveys, business outreach forums, and public-private partnerships with the local Chambers of Commerce and the EVC to identify those improvements.
- ED-2.4 Industrial Growth.** Support the retention and growth of the industrial and manufacturing sector and activity attract new industrial businesses.
- ED-2.5 Job Fairs.** Work with the local Chambers of Commerce, CVEP, and EVC to host an annual local jobs fair to facilitate the awareness of employment opportunities for both the local labor force and local businesses.
- ED-2.6 Attract Livable Wages.** Continue to pursue and attract companies and industries who offer livable wages and medical benefits for employees in Indio.

Goal ED-3: Workforce Development. A City with a highly-educated, skilled, and adaptable workforce that is growing.

ED-3 Policies

- ED-3.1 Economic Development and Labor Pool Alignment.** Align economic development efforts with the labor pool to increase the number of jobs filled by Indio residents.
- ED-3.2 Training.** Collaborate with the College of the Desert and other appropriate entities to identify and strengthen educational and training courses in new and emerging technologies that are compatible with the City's economic development policies and increase the economic competitiveness of the local labor force.
- ED-3.3 Trades.** Attract trade colleges and training programs that can prepare the local workforce for occupations in new and emerging industries.
- ED-3.4 Schools.** Collaborate with the local K-12 school districts to provide students in their STEM (Science, Technology, Engineering and Math) plus the arts and humanities courses (STREAM) with summer and part-time internship opportunities with local businesses and government agencies.
- ED-3.5 Internet Access.** Support the availability of community-wide WI-FI and high-speed internet connectivity for both businesses and households.

Goal ED-4: Fiscal Sustainability. A fiscally-sustainable community with opportunities to maximize public revenue.

ED-4 Policies

- ED-4.1 Economic Development Performance Monitoring.** Apply accepted fiscal analysis methods and establish a system of fiscal performance measures to evaluate the ongoing revenue and cost performance of economic development in Indio; regularly monitor measures such as sales tax, property tax, and other major revenue sources in order to track fiscal trends.

Goal ED-5: Infrastructure Financing. A City that uses creative and innovative infrastructure financing options to meet the needs of current and future residents and employees.

ED-5 Policies

- ED-5.1 Implement Economic Vision.** Expand the role of the Citizens Finance Advisory Commission to include assisting with the implementation of the City's economic vision, monitoring progress, and recommending effective revisions to ongoing programs and actions.
- ED-5.2 Infrastructure Financing Districts.** Explore the feasibility of establishing one or more targeted infrastructure financing districts in Indio to facilitate public capital improvements. This would

entail evaluating existing options, such as the newly adopted legislation for an enhanced Infrastructure Financing District, or other options that may become available.

- ED-5.3 Infrastructure Capital Needs Linking.** Coordinate the public infrastructure capital needs with Indio’s annual Capital Improvement Program (CIP) to create effective linkages between the General Plan’s economic development objectives and the timely provision of new or refurbished public infrastructure.
- ED-5.4 Grant Funding.** Partner with local and regional agencies and organizations, such as CVAG, CVEP and EVC, to identify and obtain federal, State, and regional grants for public infrastructure that reinforce the General Plan goals for economic development, jobs-housing balance, improved accessibility, and community sustainability.
- ED-5.5 Infrastructure Financing Sources.** Continue to identify, update, and apply traditional financing techniques, such as outside grants and loans, development impact fees, special assessments, and special taxes to maximize Indio’s access to infrastructure financing sources.
- ED-5.6 New Financing Techniques.** Continue to evaluate and implement emerging financing techniques as they become available.
- ED-5.7 Public-Private Broadband Partnership.** Actively seek a public-private partnership to provide ultra, high-speed fiber optic communications to businesses in Indio. When feasible and applicable, new construction shall install future cables to support citywide broadband internet accessibility.

Goal ED-6: Tax Base Diversification. A City with a diversified tax base that allows it to flourish during different market cycles.

ED-6 Policies

- ED-6.1 New Retailers.** Where appropriate support the entry of new retail uses into the City, including uses that will attract spending from visitors and residents of neighboring communities in the Coachella valley
- ED-6.2 Retail Development.** Support retail development that meets the community needs of Indio’s residents in appropriate locations in order to prevent the leakage of local expenditures to neighboring communities.
- ED-6.3 Transient Occupancy Taxes.** Encourage the development of uses that generate transient occupancy taxes. Require operators of short term accommodations to comply with regulations that generate transient occupancy taxes.
- ED-6.4 Non-Retail Taxes.** When possible, identify and attract industrial and warehousing/distribution businesses that have substantial business-to-business or other non-retail taxable sales potential.

Goal ED-7: Retail and Services. A City with the destination hospitality, retail, and entertainment opportunities to meet the needs of visitors and residents.

ED-7 Policies

- ED-7.1 Hospitality Inventory.** To assess hospitality needs, prepare a current hospitality facility inventory that includes the types of hotels/motels, number of rooms, price structure, location, amenities and other associated resort-style facilities.
- ED-7.2 Hospitality Opportunities.** Continue to work closely with the Greater Palm Springs Visitors and Convention Bureau to identify both hospitality businesses and locational opportunities for the Eastern Coachella Valley.
- ED-7.3 Music Events.** Continue to support local music festival event promoters and work to extend opportunities to increase development opportunities that would complement the festivals.
- ED-7.4 Cultural and Art Events.** Continue to publicize cultural and arts events and attractions in Indio to residents, potential visitors and business prospects.
- ED-7.5 Cultural Tourism.** Recognize the value of music, dance, theater and visual arts to Indio's quality of life and economy, supporting cultural tourism as part of an economic development strategy for the City.

Goal ED-8: Jobs-Housing Balance. A City that grows with a balance of jobs and housing to meet the needs of current and future residents.

ED-8 Policies

- ED-8.1 Transportation Connectivity.** Coordinate transportation investments to create better connectivity between residences and job locations.
- ED-8.2 Employment Infill.** Allow employment generating land uses at infill sites and in development locations
- ED-8.3 Missing-Middle Housing.** Encourage the growth of diverse housing types including missing-middle housing opportunities in Indio.

Implementation Actions

The table below identifies programs, policy updates, planning efforts, coordination efforts, and other actions that will help implement the General Plan’s land use and urban design vision and policies. Programs are consistent with this chapter’s goals and policies.

Table 5-1: Economic Development Programs

	DESCRIPTION	PRIORITY	TIME FRAME	RESPONSIBILITY
1	<p>Update Economic Development Planning. Continue to update and implement an economic development strategy. The strategy should be based on analysis, community and stakeholder engagement, General Plan goals and policies, and City Council priorities. In addition to the vision, goals, objectives, and actions generated during preparation of the strategy, it should actively seek to achieve the goals of the General Plan, and it may provide or address the following:</p> <ul style="list-style-type: none"> • A business visitation program • A communications strategy to market Indio to existing businesses, residents, and potential new business • Specific business types to target for business attraction efforts • An incentives policy • Measurable objectives for local economic development efforts and for collaboration with regional economic development partners • Economic development performance benchmarks and targets <p>A mechanism to measure performance and adjust programs where needed to improve performance.</p>	High	Ongoing	Economic Development, community partners
2	<p>Establish and implement an annual business visitation program. Focus on attracting and recruiting new investment in key sectors with a focus on hospitality and retail.</p> <ul style="list-style-type: none"> • Participate in investment events such as ICSC conferences • Commission a hospitality market study and identify sites for future investment • Maintain and update inventory of sites for commercial development 	High	Ongoing	Economic Development, community partners

3	<p>Establish, update, and publish an inventory of sites and facilities. Continue to make inventory of available spaces for economic development and new investments available. Keep information up to date and available on line.</p> <ul style="list-style-type: none"> • Provide information of commercial development sites by type of land use • Provide information on large scale residential developments 	High	Ongoing	Economic Development, community partners
4	<p>Downtown Indio Specific Plan. Market the Downtown Specific Plan to developers and businesses generally conducive to a pedestrian-oriented Downtown area, such as a mix of local and regional-serving retail, local serving office uses, arts and cultural facilities, and cafes / restaurants.</p>	High	Ongoing	Economic Development, Planning
5	<p>Midtown District. Market the Midtown District to developers and businesses to revitalize the area around the Fashion Fair Mall.</p>	High	Short	Economic Development, Planning
6	<p>Streamline Development Permitting Process. Work to find efficacies and make development permitting as simple and as transparent as possible.</p> <ul style="list-style-type: none"> • Move elements of permitting process on line where possible • Integrate the permitting and planning with GIS capabilities to aid in program development • Work to reduce multiple visits per applicant 	Medium	Ongoing	Planning
7	<p>Marketing and Placemaking Programs. Develop marketing and beautification strategies to raise awareness of areas targeted for revitalization.</p>	High	Short	Economic Development, Planning
8	<p>Workforce Training Inventory. Conduct an inventory of workforce training needs of businesses in Indio and coordinate with regional partners and other educational institutions to identify or create suitable training programs and help coordinate training resources in the County.</p>	High	Short	Economic Development
9	<p>Public-private partnerships. Support the use of public-private partnerships to foster job growth and vocational training, including partnerships used or planned by major public entities in Indio.</p>	Medium	Ongoing	Economic Development





chapter six

HEALTH AND EQUITY

The purpose of the Health and Equity Element is to protect the people and assets that make Indio a great place to live, work, and recreate. Place, income, living conditions and education are all significant contributing factors to health and equity in a community. How we plan and prioritize growth for the future can determine the well-being of residents in the long term. The City's physical, social, and economic environments combine to make specific individuals and groups more vulnerable to long-term changes and short-term shocks. From global climate change to economic transformation, the City has the opportunity to make choices that will positively impact the community, environment, and economy and make it more healthy, sustainable, and resilient to future change.

Key Considerations and Strategies

California law requires that a General Plan include elements (or chapters) specifically addressing environmental justice. This Health and Equity element was prepared to meet the requirements California adopted with Senate Bill 1000 (SB 1000), or the Planning for Healthy Communities Act. SB 1000 requires cities to develop an Environmental Justice element, or related environmental justice goals and policies to reduce the unique or compounded health risks in “disadvantaged communities.” Elements may address the reduction of pollution exposure, the improvement of air quality, and the promotion of public facilities, food access, safe and sanitary homes, and physical activity in disadvantaged communities.

The Health and Equity Element supports equitable development practices that are responsive to disadvantaged communities and that protect all members of a community from environmental harm and risk. Community-centered planning and design also elevates the values and priorities of historically-underserved populations, including lower-income residents and people of color, in guiding community development. In supporting and enhancing opportunities for meaningful engagement, the General Plan process recognizes the importance of addressing the intersection of health, equity, and environmental justice in creating access and expanding opportunities for all members of the community.

Public Engagement

The community engagement strategy in Indio involved ongoing community outreach, including a series of public workshops, a community questionnaire, pop-up outreach efforts (such as community blackboards), a student essay contest, and also informal observations provided by City staff. As part of this process, many community residents, along with City Council members, Planning Commissioners, General Plan Advisory Committee members, and other stakeholders shared their ideas about the opportunities and issues facing Indio, as well as how they envisioned the future of the City. The goal of the community engagement strategy was providing diverse opportunities for participation and for community members to share firsthand experiences with issues and opportunities in the City.

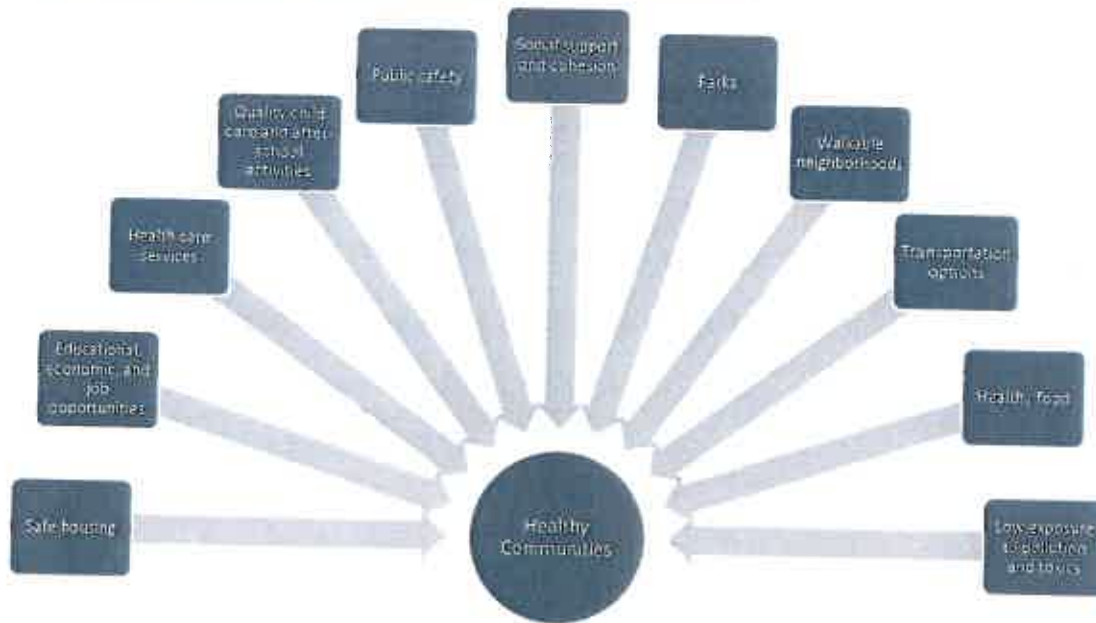


Healthy Communities

On February 7, 2018, the Indio City Council adopted a Healthy Eating Active Living (HEAL) Cities Campaign. The HEAL Cities campaign focuses on improving health across three policy areas: employee wellness, access to healthy food, and land use. The City of Indio is committed to reducing and preventing obesity and promote healthier lifestyles for all residents.

A community's overall health depends on many factors. Eating well, staying active, and seeing a doctor all influence health. Health, however, is also influenced by access to social and economic opportunities, the quality of education, neighborhood conditions, workplace safety, and the cleanliness of water and air, among others. These conditions influence why some people are healthier than others, and why where people live and work matter to community health. Figure 6-1 shows examples of key healthy community components.

Figure 6-1: Example of Healthy Communities Components



Because healthy communities are influenced by a wide variety of factors, strategies to improve health are woven throughout the General Plan. Examples include:

- Promoting development of non-polluting industries that are not major sources of air and water pollution (Land Use and Community Design Element, Goal 10).
- Encouraging the use of bicycles and walking as the preferred transportation mode for trips to and from schools by implementing a "Safe Routes to School" Program (Mobility Element, Goal 2).

- Collaborating with the College of the Desert and other appropriate entities to identify and strengthen educational and training courses in new and emerging technologies (Economic Development Element, Goal 3).
- Focusing City efforts to create a variety of new parks types, including mini, neighborhood, and community gardens within Indio's existing neighborhoods, particularly the most disadvantaged neighborhoods (Parks, Recreation, and Open Space Element, Goal 2).

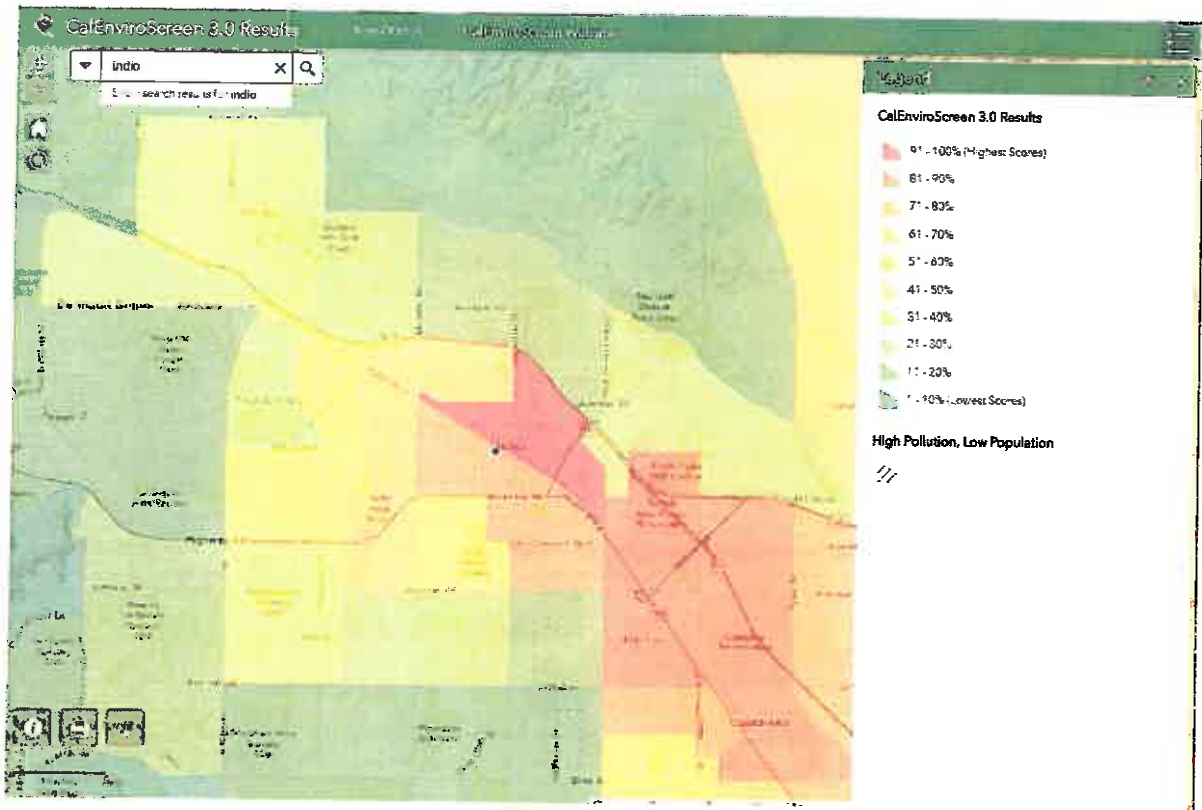
Encouraging a healthy community can bring many benefits to a community, from better health to new jobs to economic investments. Improving these determinants can also help promote equity and community development over the long term.

Disadvantaged Communities

While the promotion of healthy communities can bring many benefits to all members of a community, the impacts of environmental burdens and poor planning can have disproportionate impacts on disadvantaged communities, including low-income residents, minority populations, communities of color, and tribal nations. The California Environmental Protection Agency (CalEPA) defines a disadvantaged community as an area that is a low-income area and that is disproportionately affected by environmental pollution. The CalEnviroScreen (CES) tool can be used to identify disadvantaged communities disproportionately impacted by environmental pollution and other hazards. The CES tool provides a data source for identifying communities burdened by multiple sources of pollution, based on environmental exposures, environmental effects, and population characteristics, including health outcomes and socioeconomic data.

As shown in Figure 6-2, the CES tool identifies various communities in Indio as "most disadvantaged." Based on the scores, the City has relatively high levels of ozone, which is compounded by a concentration of neighborhoods and individuals that struggle with high levels of poverty, low levels of educational attainment, and poor health conditions.

Figure 6-2: CalEnviroScreen (CES) Tool



A second tool, the California Healthy Places Index (HPI), can be used to analyze the cumulative burden of social, economic, and environment conditions that may lead to unequal, inequitable, or disparate outcomes on a specific group of neighborhoods. In addition to pollution burden, the HPI tool emphasizes a broader spectrum of social factors (or determinants) that impact health, including education, housing, economic, and access to health. As shown in Figure 6-3, Indio has various neighborhoods with a low HPI score and higher proportion of residents considered especially vulnerable to public health risks, including children, the elderly, the disabled, recent immigrants, and those in poverty. Additionally, Figure 6-4 provides a map of disadvantaged communities in the lowest quartile of the HPI, identified by census tract.

Figure 6-3: Healthy Places Index (HPI)

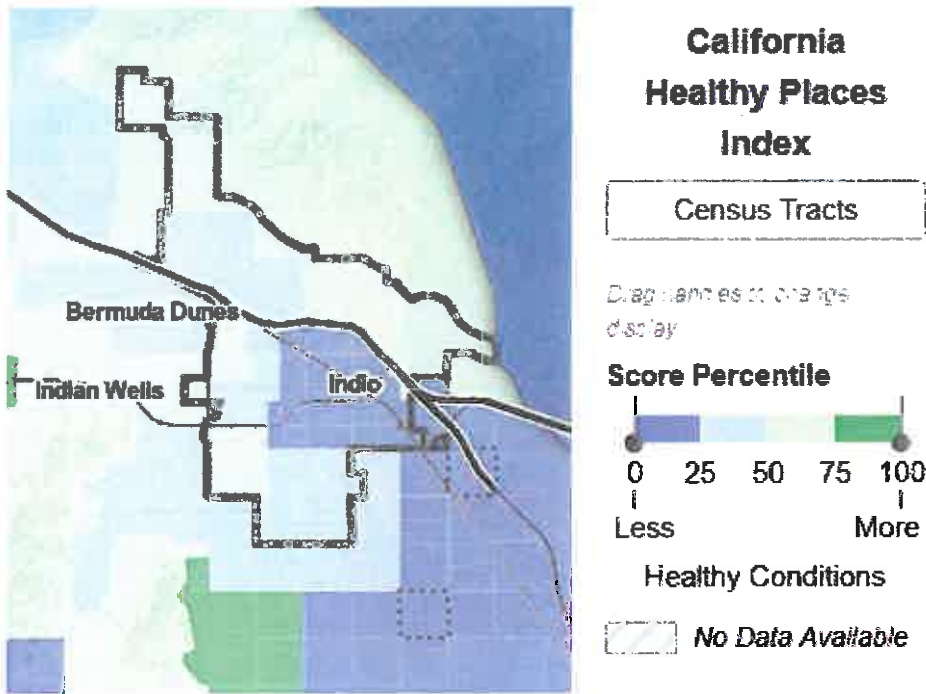
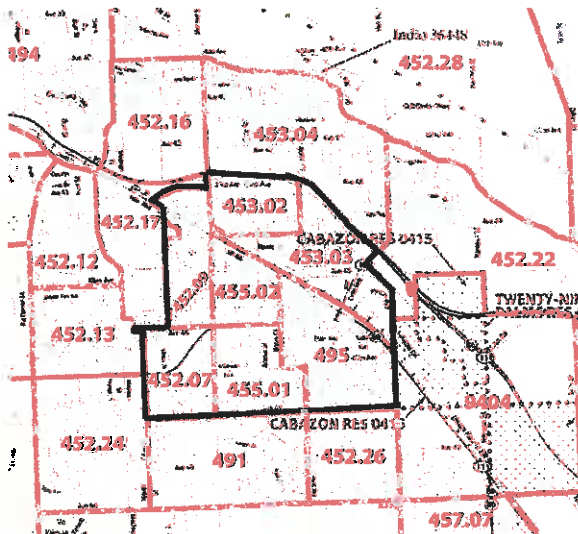


Figure 6-4: Indio Census Tract Map



Poverty consistently co-exists with a wide variety of negative health outcomes. Table 6-1 provides a comparative profile of disadvantaged communities in the City of Indio. While census tract 9404 includes an area within the City, it is largely within Coachella and has not been included within the Indio Health Profile. In Indio, poverty disproportionately impacts families with children. Families with children fare poorly, with poverty rates much higher than the rates for all families, particularly in neighborhoods

identified by the HPI as disadvantaged communities. This is particularly important because children in poverty can experience learning difficulties, and social, emotional and behavioral problems. In general, children are also especially susceptible to asthma from air pollution.

Residents in disadvantaged communities also face health risks from overcrowding, and if recent immigrants, may also be less likely to seek health services out of fear of legal repercussions due to their immigration status. Language barriers can lead to miscommunication, but also feelings of social isolation and solitude. Compared to the rest of the City, all census tracts identified by the HPI as disadvantaged communities have over one third or residents that identify with speaking English less than 'very well.' Issues of social isolation and a lack of physical activity can also impact an aging population. While the number of residents age 65 and over are generally lower in disadvantaged communities, there are two census tracts (452.09 & 455.02) where the number of residents age 65 and over living alone are higher than the rest of the City. For the City, these neighborhoods present an opportunity to enhance social relationships and improve physical health for these residents.

Table 6-1: Indio Health Profile

	Census Tract							Indio City
	452.07	452.09	453.02	453.03	455.01	455.02	495	
Demographic								
Under age 5	10.5%	6.0%	6.6%	6.9%	6.9%	11.2%	7.3%	7.1%
Under age 18	22.5%	24.6%	21.7%	26.6%	20.7%	17.7%	22.6%	20.2%
Age 65 and over	9.1%	10.7%	14.8%	7.4%	7.3%	10.3%	9.4%	15.7%
Age 65 and over living alone	5.9%	14.9%	5.3%	7.1%	2.1%	10.7%	5.4%	8.8%
Average household size	3.2	3.25	3.92	3.53	3.31	3.23	3.38	3.1
Non-White (including Hispanic or Latino)	66.9%	62.7%	54.5%	69.0%	68.5%	57.9%	56.6%	49.1%
Percent high school graduate or higher	71.4%	61.2%	57.4%	48.2%	64.3%	51.7%	54.0%	74.2%
Percent bachelor's degree or higher	7.1%	7.0%	3.2%	2.6%	5.0%	8.1%	9.6%	16.0%
Speaks English less than 'very well'	31.8%	33.2%	39.1%	38.5%	30.3%	37.1%	43.3%	24.0%
Percentage of families whose income in the past 12 months is below the poverty level								
All families	26.0%	27.3%	22.1%	38.6%	16.1%	25.8%	15.9%	14.0%
With related children under 18	30.1%	40.5%	24.0%	54.0%	26.2%	43.1%	22.3%	22.2%
With related children under 5	54.5%	42.7%	-	46.0%	27.3%	23.9%	0.0%	19.1%
Employment								
Percent Unemployed	10.4%	12.0%	8.8%	13.7%	9.5%	4.2%	8.2%	7.2%
Health Insurance								
No health insurance coverage	23.5%	24.3%	25.7%	27.4%	24.7%	30.7%	23.0%	18.1%

Source: 2012-16 American Community Survey 5-year estimates

Broader Context

A guiding principle of environmental justice is that all members of a community should benefit from understanding the risks of environmental harms and the protections to mitigate their impacts. The built environment actively shapes health outcomes in communities. Although conventional planning practices (such as separating residential and commercial uses, building low density areas, constructing streets primarily for automobiles, and not providing adequate transportation choices) are not the single cause of chronic health problems in the United States, there is increasing documentation that they are often a contributing factor. Research indicates that auto-oriented, low-density, single-use places – as well as places underserved by parks and active recreation facilities – discourage physical activity and therefore contribute to an increased risk of heart disease, cancer, stroke, and diabetes.

As part of its commitment to the Healthy Eating Active Living (HEAL) Cities Campaign, the General Plan will build on existing efforts to encourage physical activity and access to healthy food in the City. For instance, the City has begun improving existing streets and sidewalks to encourage walking and biking and is currently developing a Safe Routes to School Master Plan, conducting a Multi-Modal Feasibility Study, and creating a five-mile hiking trail in the northern part of the city.

The following section provides a snap-shot of several key health outcomes and issues in the City.

Health Status

Based on data from UCLA's *California Health Interview Survey* (CHIS), the City of Indio has a low rate of children and youth (5.3%) that report fair or poor health. In contrast, adults (18-64) in the City (21.8%) and older adults (65+) in the County of Riverside report a higher rate of being in fair or poor health. Age is also a major risk factor for these chronic diseases. Because Indio has a younger-than-average population, there may be residents with unhealthy lifestyles that are at high risk for developing these chronic diseases in older age, but that have not yet been diagnosed. Treatment and diagnosis is often hampered by lack of preventative care, lack of insurance, and poor access to healthcare.

The data also suggests that obesity may be a critical public health problem for Indio. This is borne out by the fact that 28.4% of adults in Indio are obese or overweight, compared to 26.1% of residents in Riverside County and 25.8% in the state of California. The City's health challenges also include high rates of diabetes and death rates from heart disease, cancer, and chronic lower respiratory disease/chronic obstructive pulmonary disease. These diseases are generally linked to unhealthy weight, poor nutrition, and physical inactivity.

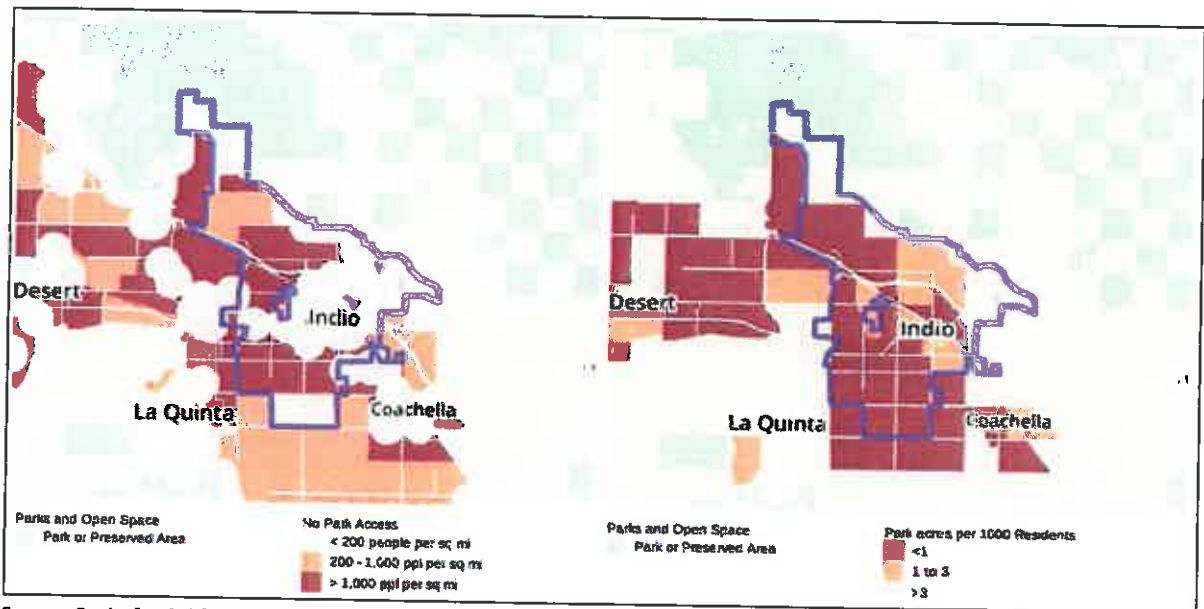
Life Expectancy

Life expectancy is an overarching health indicator for any population. Leading causes of death can highlight a population’s risk factors and suggest policy priorities. Average life expectancy in zip code 92201 is 82 years, 3 years less than in 92203. There is no single cause for this disparity; rather, it suggests that the average Indio resident faces a variety of combined physical, social, and economic conditions that reduce health status and life expectancy. This disparity in life expectancy underscores why improving community health is a critical long-term goal.

Barriers to Physical Activity

The high rates of overweight and obese residents are compounded by a high percentage of adults (29.3%) in the City that do not participate in any physical activity. Generally, the City has a good pedestrian network. However, many transportation corridors – such as roadways, freeways, and train tracks – are difficult and dangerous for pedestrians to cross and thus discourage physical activity. In parts of the City, the prominence of suburban street design is not conducive to pedestrian activity. Residents’ ability to bicycle, walk, and exercise for recreation is also limited by a relative lack of public parks and recreation facilities, a cohesive bicycle network, and the distance many residents must travel to reach such facilities.

Figure 6-5: Park Access



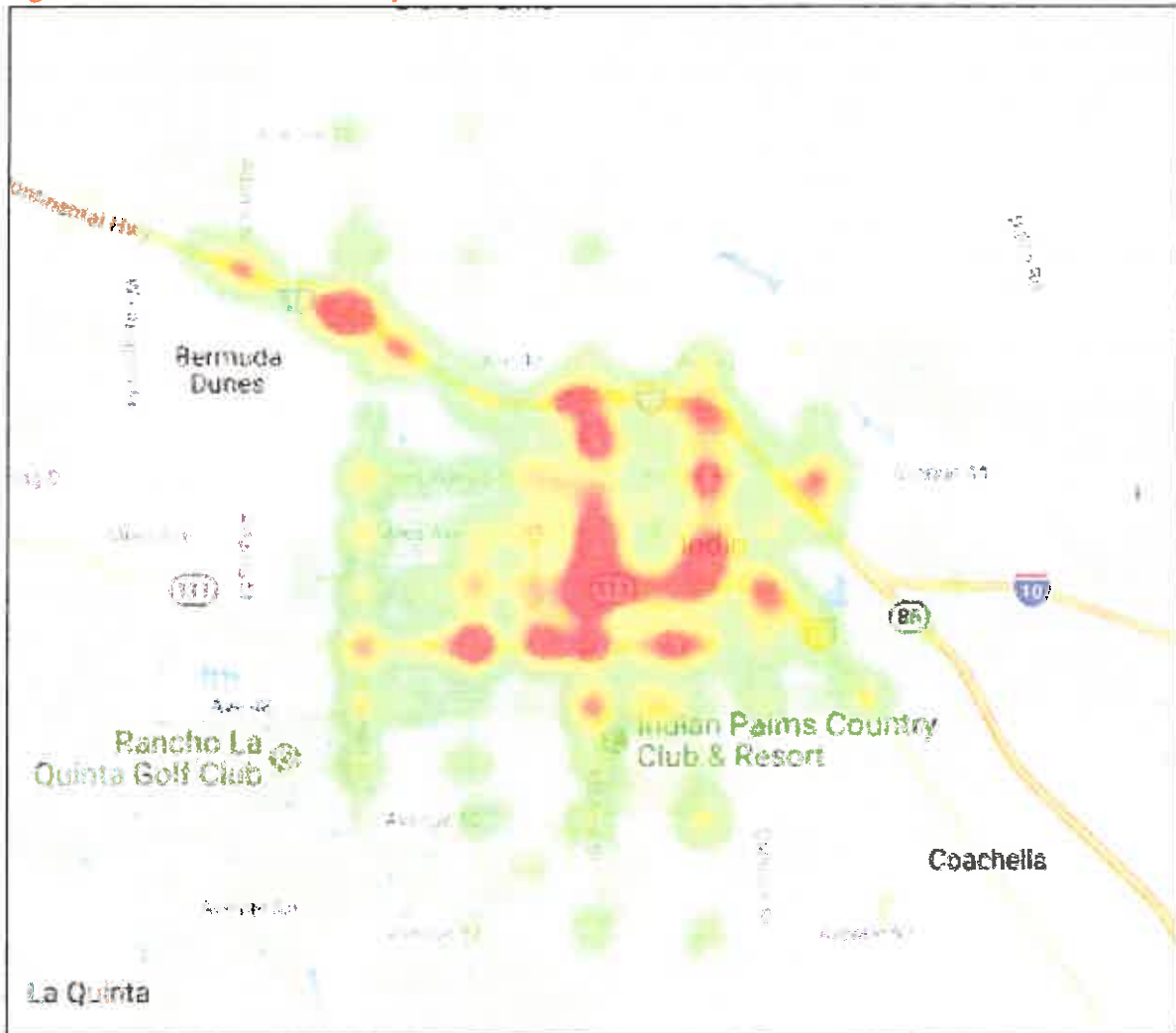
Source: Parks for California

While approximately one-third (33.5%) of Indio residents report walking at least 150 minutes a week, a limiting factor to physical activity levels is restricted access to public recreational and open space, as shown in Figure 6-5. Across the City, 53% percent of residents live further than half a mile from a public park. Despite the high density of people living in the City, 93% of residents live in areas with less than 3 acres of public parks or open space per thousand residents. Lastly, the bicycle network in Indio is incomplete and certain areas lacks infrastructure, a fact which limits the ability of residents to use bicycles for commuting, daily needs, or recreational purposes as illustrated in Chapter 4.

Street Safety for Walking, Driving, and Cycling

Concerns about pedestrian, bicycle, and vehicle safety in Indio are amplified by the number of traffic collisions along the City's largest streets. This is especially true of areas with high pedestrian and vehicle traffic. Based on data provided by the Transportation Injury Mapping System, there were a total of 1,008 traffic collisions in Indio from 2010 to 2014. Of these, 9% involved a pedestrian and 25 (2%) resulted in fatalities. The locations of these accidents are shown in Figure 6-6.

Figure 6-6: Vehicle Collision Map in Indio



Source: Transportation Injury Mapping System

The majority of injury and fatality incidents occurred along Indio's high-volume streets, most notably Monroe Street, Dr. Carreon Boulevard, and Highway 111, but also along the I-10 corridor, Jackson Street, and Avenue 48. The majority of pedestrian collisions took place at crossings and not in crosswalks (45.6%).

Nutrition

Better access to nutritious food and more opportunities for physical activity could reduce Indio residents' vulnerability to chronic diseases and is a key component of the HEAL Campaign. Poverty, low levels of education, and lack of access to health care may also contribute to these health conditions. As noted above, Indio has a high rate of obesity and overweight residents. Poor diet is likely a significant cause of this trend. According to the Centers for Disease Control, 82% of adult residents eat less than five servings of fruits and vegetables a day. One reason people may consume higher levels of unhealthy food is that it is often cheaper and easier to access than healthier food options. In the period between 2009-2014, the number of fast food restaurants increased (10%) across Riverside County. There is a significant number of retailers selling food with low nutritional values across the City and only one farmers' market exists in the City.

Food security is also important for community health and wellbeing. "Food security" is defined as having access to enough food for an active, healthy life for all people at all times. Food insecurity can lead to undernourishment and malnutrition, which coincide with fatigue, stunted child development, and other health issues. Food insecurity among low income households is significantly higher in Indio (11.4%) than at the County (7.6%) and State (8.1%) levels. Households that lack "food security" can obtain supplemental assistance from government programs, such as SNAP and the Women Infants and Children (WIC) program. These vendors are generally located along Jackson Street and Highway 111.

Air Quality

The City of Indio is located within the Salton Sea Air Basin (SSAB). Within this Basin, ozone and particulates are the pollutants that pose the most significant threat to human health. Ozone and fine particle matter are difficult to detect with the naked eye, so it is not always apparent when there is poor air quality. Ozone is a gas composed of three oxygen atoms. It is formed when volatile organic compounds (VOC) combine with nitrogen oxides in the presence of ultraviolet sunlight. Inhaling ground-level ozone can result in a number of health effects that are observed in broad segments of the population. People may experience symptoms such as coughing, throat irritation, pain, burning, or discomfort in the chest when taking a deep breath, and chest tightness, wheezing, or shortness of breath. Particulate matter (PM) consists of a mixture of dry solid fragments and small droplets of liquid. These particles are linked with increases in mortality rates, respiratory infections, and the number and severity of asthma attacks.

Air quality in Indio is regulated by the South Coast Air Quality Management District (SCAQMD), which has jurisdiction over the South Coast Air Basin (SCAB) and the desert portion of Riverside County in the SSAB. The District monitors air quality at two permanent locations: one in Palm Springs and one in Indio. The stations assess PM2.5, PM10, ozone, carbon monoxide, sulfur dioxide, and nitrogen dioxide levels.

The Coachella Valley has a history of elevated PM levels, which are closely associated with fugitive dust emissions from construction activities and the area's natural wind processes. The Coachella Valley is currently designated as a serious non-attainment area for PM10, which refers to particulate matter that is larger than 2.5 micrometers, but smaller than 10 micrometers. Coachella Valley governments, agencies, private and public stakeholders, and the SCAQMD, have proactively worked to reduce unhealthy levels of PM10 dust. In response to high levels of dust, SCAQMD adopted the State Implementation Plan for PM10 for the Coachella Valley (CVSIP), which outlines reasonably available control measures and establishes a future attainment date for areas previously unable to meet federal PM10 standards.

Ozone levels have been declining; however, concentrations have exceeded State and federal standards in recent years. In 2014, there were 30 days where the monitoring station in Indio registered an exceedance of the State's eight-hour concentration standard and 61 days where the monitoring station in Palm Springs registered exceedance of the State's eight-hour concentration standard. In the same year, the federal eight-hour concentration standard was exceeded 10 days at the Indio monitoring station and 35 days at the Palm Springs station.

Periodically, the SCAQMD prepares an update to the Air Quality Management Plan to comply with federal requirements and/or incorporate the latest technical planning information. This is a regional and multi-agency effort to develop strategies primarily focused on addressing ozone and PM standards. The SCAQMD also offers assistance to government policy-makers in adopting and implementing local air quality policies. This Element provides Indio with goals and policies to improve its air quality. The City has also prepared a Climate Action Plan, which establishes current emissions and sets targets for reductions in greenhouse gases that will improve overall air quality.

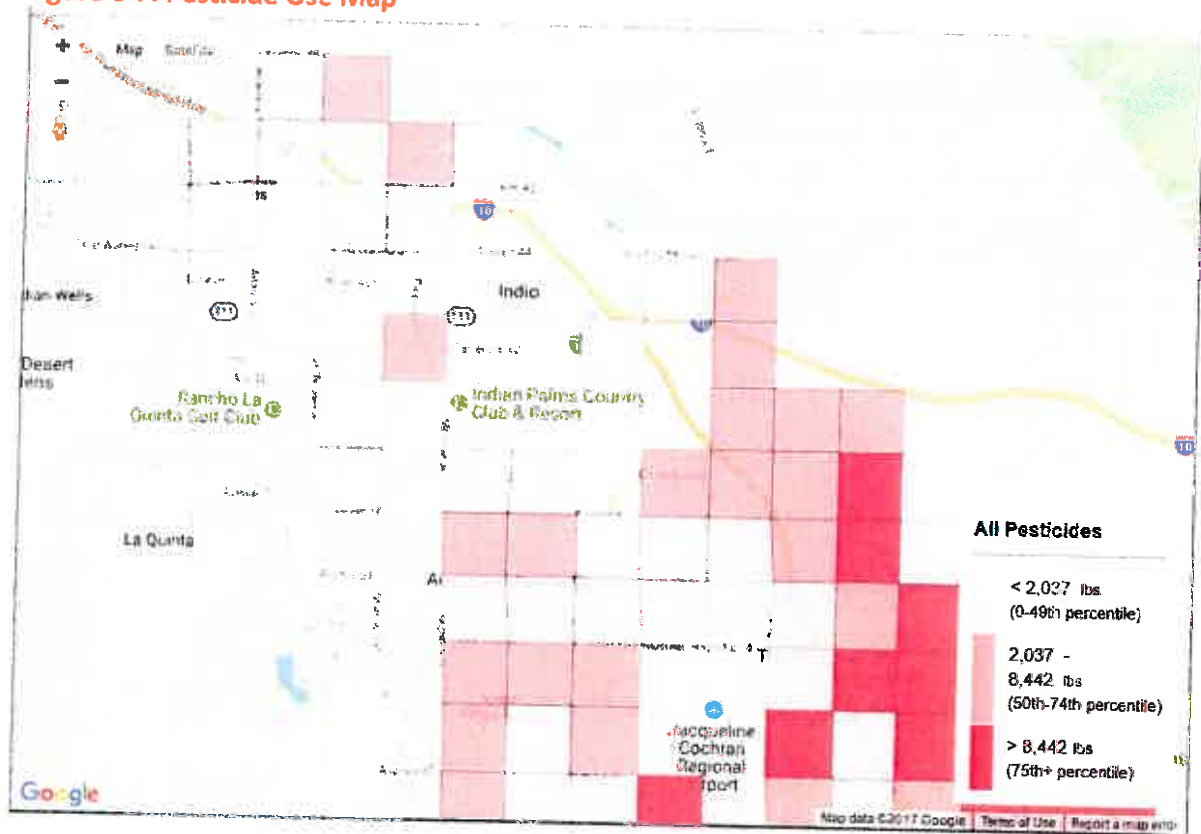
Indio's reported rates of adult asthma are higher in areas south of the 10 freeway, where many residents live near sources of air pollution – industrial lands, truck routes or freeways – that contribute to respiratory illnesses. It is also possible that the City's lower than average level of access to health insurance, information and care leads to under-diagnosing and under-reporting of respiratory disease.

Environmental Concerns

Various environmental concerns also negatively impact the health of residents and Indio's food system. Pollution from agricultural fertilizers and pesticides are primary sources of groundwater contamination in Indio. While contaminants can be naturally occurring or manmade, these can impact both the environment and human health.

Pesticides, for instance, are used to prevent and control pests, weeds, and other plant pathogens. Exposure to pesticides can happen by touching treated surfaces (skin contact), eating or drinking contaminated food or water (ingestion), or by breathing it in (inhalation). Areas with higher concentrations of pesticide use are shown in Figure 6-7.

Figure 6-7: Pesticide Use Map



A single or short-term high-level exposure can result in poisoning or illness. Similarly, chronic, or long-term exposure, can result in serious illness, including some types of cancers, pregnancy issues, abnormal births, and poor brain development. Symptoms can include headaches, dizziness, and allergies to chemical burns on the skin, unconsciousness, and death. In Indio, pesticide use includes Chloropicrin, 1,3-Dichloropropene, Sulfur, and Mancozeb.

Additionally, occupational exposure can put agricultural workers in particular risk for pesticide-related illnesses. Children and pregnant women are at greater risk from the health effects of pesticide exposure. Exposure can also be a problem in disadvantaged communities where the risk to in-home pesticide use is high and where community residents may have little or no participation in pest control decisions.

The effects of short or long-term exposure can be compounded by industrial manufacturing, agricultural run-off, and wastewater discharge. Chromium-6, a cancer-causing chemical is found in Indio's tap water at a rate of 13.1 parts per billion, while California's Public Health Goal for chromium-6 is 0.02 ppb. The adverse effects of drinking or inhaling Chromium-6 include lung cancer, kidney damage, and occupational asthma.

Access to Healthcare and Health Facilities

Access to health care is a major health policy issue in Indio. Many people cannot afford health care and there is a limited number of health providers in the area. In 2014, 26.9% of adults between the age of 18

and 64 were uninsured in Indio. A lack of access to health care can further complicate chronic diseases, include heart disease or diabetes, which are already prevalent in the City.

There are also a limited number of health care facilities in the City. John F. Kennedy Memorial is the only hospital in the City. Other nearby facilities include Eisenhower Medical Center in Rancho Mirage, Kaiser Permanente in Palm Desert, and Desert Regional Medical Center in Palm Springs.

There are neighborhoods of the City, particularly those concentrated north of Avenue 46, that are located within a Health Professional Shortage Area (HPSA) for primary care and mental health professionals. A Health Professional Shortage Area is a federal designation given to areas that demonstrate a shortage of healthcare professionals, which may include primary care professionals, dentists, and mental health professionals.¹ Additionally, many of Indio's neighborhoods have also been designated as a Medically-Underserved Area (MUA).^{2, 3}

¹ California Office of Statewide Health Planning and Development (2015). Health Professional Shortage Areas.

² MUAs are determined through a ratio of primary medical care physicians per 1,000 population, infant mortality rate, percentage of the population with incomes below the poverty level, and percentage of the population age 65 or over.

³ U.S. Department of Health and Human Services.

Goals and Policies

Healthy Neighborhoods

Promote community health programs, services, and design that improve health outcomes by expanding and leveraging resources and capacity. Health Equity can increase access to medical care and social and economic opportunities.

Goal HE-1: Health Equity. A City that supports health equity for all residents by promoting access to affordable, quality health care, mental health care, and social services.

HE-1 Policies

HE-1.1 Health Equity. Encourage programs and practices that aim to reduce health inequities. Partner with public and private health partners to work towards ensuring a healthy and sustainable lifestyle for all Indio residents and workers.

HE-1.2 Health in All Policies Approach. Integrate a Health in All Policies approach to governance that aligns with County initiatives, policies, and strategies.

HE-1.3 Partnerships for Health. Support partnerships with local health service providers, the County Public Health Department, schools, and other community-based organizations to promote health, wellness, and preventative care.

HE-1.4 City Actions and Programs. Implement the HEAL Campaign by promoting and addressing the health and wellness of the public through the City's actions, policies, programs and publications.

HE-1.5 Educational Materials about Health. Provide information about existing health care facilities through City-sponsored events, publications and activities.

HE-1.6 Flexible Spaces. Promote the flexible use of community centers, libraries, and schools to provide important health services to Indio residents. Consider novel health care alternatives, like pop-up vaccinations or pop-up health screening.

Goal HE-2: Healthy Neighborhood Design. Neighborhoods designed to encourage a healthy lifestyle for people of all ages, income levels, and cultural backgrounds.

HE-2 Policies

HE-2.1 Neighborhood Design. Design neighborhoods to promote pedestrian and bicycle activity as alternatives to driving. This policy is implemented through the Land Use and Community Design Element.

HE-2.2 Complete Streets. Prioritize transportation system improvements that encourage activity by all users, particularly in the areas with the highest need. This policy is implemented through the Mobility Element.

HE-2.3 Universal Park Access. Increase access to existing and proposed parks and open spaces. Apply universal design principles to ensure that people of any culture, age, size, weight, race, gender,

and ability can experience an environment that promotes their health, safety, and welfare. This policy is implemented through the Parks, Recreation, and Open Space Element.

HE-2.4 Sidewalk Network. Design neighborhoods to facilitate social connectivity and walkability. This policy is implemented through the Mobility and the Land Use and Community Design Elements.

HE-2.5 Community Engagement. During future neighborhood planning processes, encourage continuous participation by those who will be affected by the plan, including residents, property owners, and businesses, as well as the general public and interested groups. If possible, pursue public-private partnerships to implement projects and ensure long-term sustainability and benefits to the community.

HE-2.6 Coordination with Regional Agencies. Integrate a Health in All Policies approach to governance that aligns with County initiatives, policies, and strategies.

HE-2.7 Health-Promoting Uses. Prioritize health-promoting uses in new development including neighborhood markets, grocery stores, medical centers, pharmacies, parks, gyms, and community gardens.

Environmental Factors Affecting Health

Clean air, water, and soil are critical for Indio residents. Other effects on health include access to food, quality education, and safe neighborhoods. The following goals and policies aim to reduce negative environmental factors that contribute to poor health for Indio residents.

Goal HE-3: Environmental Quality. A City designed to improve the quality of the built and natural environments to reduce disparate health and environmental impacts.

HE-3 Policies

HE-3.1 Regional Air Quality Planning Efforts. Participate in air quality planning efforts with local, regional, and State agencies that improve local air quality to protect human health and minimize the disproportionate impacts on sensitive population groups.

HE-3.2 Contaminated Sites. Continue to work with the appropriate local, State, and federal agencies to promote the clean-up of contaminated sites to protect human health.

HE-3.3 Construction Pollution. Reduce particulate emissions from paved and unpaved roads, construction activities, and agricultural operations.

HE-3.4 Sensitive-Receptor Uses. Discourage development of sensitive land uses – defined as schools, hospitals, residences, and elder and childcare facilities – near air pollution sources that pose health risks – including freeways and polluting industrial sites.

HE-3.5 Truck Routes. Designate truck routes to avoid sensitive land uses, where feasible.

HE-3.6 Smoke-Free. Encourage smoke-free and Vape-free workplaces, multi-family housing, parks, and other outdoor gathering places to reduce exposure to second-hand smoke.

HE-3.7 Public Education. Provide educational information about air quality issues and their health effects, including best practices for reducing and/or eliminating sources of indoor air pollution.

HE-3.8 Sensitive Receptors and Agricultural Operations. When new sensitive receptors are proposed adjacent to existing active agricultural operations, ensure that an appropriate buffer is provided to minimize adverse impacts and that future residents will be provided with a notice specifying the potential nuisances, such as dirt, noise, odors, and slow moving agricultural machinery that would be associated with the agricultural operations.

HE-3.9 Agricultural Outreach. Work with the agricultural community to develop and distribute an informational brochure regarding best practices to reduce or eliminate surface and groundwater contamination, reduce particulate emissions from agricultural operations, minimize soil erosion, and prevent the buildup of salts in soils.

Goal HE-4: Healthy Food. Healthy and affordable food available to all residents.

HE-4 Policies

HE-4.1 Healthy Food. Ensure convenient access to affordable, fresh produce in all neighborhoods, including grocery stores, farmers' markets, and community gardens, particularly in underserved areas of Indio.

HE-4.2 Fast Food Access. Discourage unhealthy fast food outlets from locating near schools.

HE-4.3 Healthy Food Awareness. Promote public awareness and education about the importance of locally-produced food and food security.

HE-4.4 Local Food Production. Encourage local food production and distribution from Riverside County to reduce the distance food travels and pollution associated with transporting food.

HE-4.5 Mobile Food Markets. Maintain regular, weekly certified farmers' market and other mobile health food markets in Indio.

HE-4.6 Youth Food Programs. Work with the Desert Sands Unified School District and other Indio schools to create or implement educational programs for kids about healthy eating, such as edible school yards and healthy cooking classes.

HE-4.7 Liquor Stores. Be cognizant of potential concentrations (e.g., multiple stores on the same block or intersection) of liquor stores and corner stores that sell liquor.

HE-4.8 Food Insecurity. Educate residents about eligibility requirements for government nutrition programs, such as WIC and CalFresh.

HE-4.9 Food Businesses. Encourage and promote innovative food microenterprises in Indio and create economic development opportunities for entrepreneurs.

Goal HE-5: Safety. Neighborhoods that enhance the safety and welfare of all residents, employers, and tourists in the City of Indio.

HE-5 Policies

HE-5.1 Crime Prevention Programs. Promote Business and Neighborhood Watch programs to help maintain a clean and safe environment.

HE-5.2 Community Volunteer Programs. Support the creation of community volunteer programs, including police cadets, Citizens Helping Indio Police (CHIP), and the Police Chaplain unit.

HE-5.3 Community and Recreational Programs. Encourage the development and operation of community and recreational facilities, programs, internships and block-party neighborhood events as pre-emptive strategies to reduce youth-related crime and to serve all phases of life (e.g., children, families, senior citizens).

HE-5.4 CPTED. Use Crime Prevention through Environmental Design strategies (CPTED) in new and existing development to improve public safety, including lighting, building orientation, and landscaping measures.

HE-5.5 At-risk Youth Programs. Continue to support programs for at-risk youth and parents of at-risk youth, such as parent training programs to prevent and intervene in destructive adolescent behavior, youth community outreach events, and the youth offender court program where juvenile offenders are tried in front of a jury comprised of their peers.

HE-5.6 Graffiti Removal. Continue the robust graffiti prevention and removal efforts through the City of Indio's Graffiti Abatement Team which is managed by the Indio Police Department. The Team removes graffiti from public property (including parks, street signs, sidewalks, etc.) or property adjacent to public rights-of-way.

Goal HE-6: Education and Workforce. High-quality educational and expanded workforce opportunities for all Indio residents.

HE-6 Policies

HE-6.1 Enrichment Programs. Promote free or low-cost child and family enrichment programs and afterschool supplemental education programs.

HE-6.2 Workforce Training. Develop public-private partnerships with automakers, telecommunications, green industry, health care, education, and other sectors to support educational and workforce training opportunities, particularly to assist vulnerable populations in competing for career opportunities in growth industries. This policy is also implemented through the goals and policies in the Economic Development Element.

HE-6.3 Early Childhood Education. Support a high quality, universal system of early childhood education, especially in low income communities.

HE-6.4 Youth Training and Employment. Support participation in youth training and employment programs as a strategy to improve educational attainment and generate professional aspirations.

HE-6.5 County Partnership. Partner with the network of Riverside County Libraries to promote educational programs that teach children, teens, and adults with low literacy to improve reading skills, improve English conversational skills, and homework support.

HE-6.6 Adult Education. Support the creation of adult education and training programs, including English language classes, vocational training, and financial literacy programs that empower residents to save, budget, build credit, and explore investment opportunities.

Implementation Actions

The table below identifies programs, policy updates, planning efforts, coordination efforts, and other actions that will help implement the General Plan’s community health and equity vision and policies. Programs are consistent with the HEAL Cities Campaign and support this chapter’s goals and policies.

Table 6-2: Community Health and Equity Programs

	DESCRIPTION	PRIORITY	TIME FRAME	RESPONSIBILITY
1	Health Resources Inventory. Develop an inventory of health resources in the City in cooperation with the Riverside County University Health System and local hospital system.	Medium	Short	Planning
2	Healthy Food Plan. Develop a plan to attract more healthy food choices for residents of the City. The plan should strategically consider location and expansion of new and existing businesses to promote access to healthy food choices, including farmers’ markets (see action 3), throughout the City, including incentives to locate or expand grocery stores and other healthy food purveyors throughout the City.	Medium	Long	Planning
3	Farmers’ Market. Pursue funding and partner with local organizations to provide a year-round farmers market in the City and seek out additional vendors to participate in the market.	Medium	Medium	Planning
4	Health Fitness Challenges. Create and implement health fitness challenges for City employees, businesses, and residents to encourage healthy living.	Low	Medium	Community Services
5	Health Fair. Host an annual Health Day Fair with information, health care screenings and services, and activities celebrating an active lifestyle. The event should be sponsored by the City and involve a wide range of service providers.	Medium	Ongoing	Community Services
6	Employee Health and Wellness Program. Implement employee health and wellness programs such as: <ul style="list-style-type: none"> • Offering discounts for gym or health club memberships; • Offering onsite health clinics for employees; • Bringing in exercise instructors to teach lunchtime classes; 	Medium	Medium	Community Services

	<ul style="list-style-type: none"> • Distributing educational information about physical fitness, nutrition, and other health topics to employees; • Organizing competitive events that encourage employees to be physically active; and • Prohibiting smoking and vaping on City property. 			
7	<p>Town Hall Meetings. Hold bi-annual town hall meetings to implement the General Plan. Publicize the meetings widely and in multiple languages.</p>	Medium	Short	Planning
8	<p>Zoning Code Updates. Amend City codes and ordinances for their impact on health. Topics that should be addressed include:</p> <ul style="list-style-type: none"> • Location of fast food restaurants and liquor stores. • Standards for improved indoor air quality resulting from pollution such as toxics, VOCs, mold, paint, asbestos, smoking, and other chemicals. • Encouragement for HVAC systems that achieve high levels of indoor air quality. • Reduction of dust and other emissions from construction sites and unpaved industrial areas of the City. • Standards for multi-family buildings that locate along freeways or truck routes or near hazardous materials. 	High	Short	Planning
9	<p>Development Checklist. Coordinate with the Riverside County Public Health Department to implement a land development review checklist to ensure that projects enhance public health outcomes. The checklist should address topics such:</p> <ul style="list-style-type: none"> • Pedestrian environment • Building siting • Access to transit • Access to parks • Proximity to healthy food sources • Proximity to existing or potential sources of pollution (such as freeways and land uses that use hazardous materials) • Shade and tree canopy 	High	Short	Planning

10	Smoke-Free Events. Support smoke-free public events and facilities, including farmers' markets, public parks and trails, and community street fairs.	Medium	Ongoing	Community Services
11	Regional Coordination. Cooperate with local, regional, and state agencies that work to improve air quality and mitigate negative health impacts.	Medium	Ongoing	Planning
12	Vandalism and Maintenance Reporting. Create a program and/or method for residents to monitor and report vandalism along with maintenance issues in parks and public Right-of-Way.	Medium	Medium	Public Works
13	CPTED Manual. Develop a development manual to provide basic requirements and incentives for the inclusion of design features in new development to reduce potential for crime. These features could include well-lighted parking areas, open landscaping, limited access into and between buildings, and limited access to rooftops.	Medium	Medium	Planning
14	Learning Partnerships. Partner with local institutions and other public and private organizations to maintain and enhance existing extended learning opportunities through after-school enrichment programs, summer boost programs, school breaks, and weekends for all children.	High	Medium	Community Services
15	Housing Element. Incorporate analysis of key livability issues, including overcrowding, lead exposure, and other substandard housing conditions, in the next housing element update.	High	Short	Planning





chapter seven

PARKS, RECREATION AND OPEN SPACE

Easy access to safe and well-maintained parks, open space, and recreation facilities is vital to Indio as a community. Quality green spaces can encourage physical activity, improve well-being, provide a respite from developed areas, serve as habitat for native species, and provide an aesthetic backdrop to the City. Taken together, such parks and natural areas can contribute to a more livable and attractive urban environment. In addition, parks and open space provide urban sanctuaries that reduce stress and promote good mental health, accommodate trees and vegetation that reduce the heat-island effect of paved developed areas. The City of Indio recognizes that parks, recreational facilities, and open space provide numerous benefits to the community while contributing to the character and charm of Indio. As the City of Indio continues to expand and develop, future demand for open space and parks will increase. The Parks, Recreation, and Open Space Element establishes long-term goals and policies for producing, managing, and maintaining public parks, citywide trails network, open space, and recreational facilities in Indio. This Element also provides an inventory of existing parks and recreational facilities, urban agricultural sites, and parkland classifications.

Key Considerations and Strategies

Well-designed and properly-maintained parks and recreational facilities promote a healthy community and enhance the quality of life for residents and visitors. Parks provide space for active and passive recreation and special events, enhance the appearance of the community, and contribute to higher property values. Natural open space can protect critical habitat and special status species, creating places for trees and other vegetation that work to improve air and water quality.

As Indio continues to grow in population, conservation of open space for ecosystems services, expanded resource conservation efforts, and passive and active recreational facilities planning is imperative. Indio's parks underserve existing, disadvantaged neighborhoods, and are not distributed evenly across the City. Indio provides 1.2 acres of City-owned parkland per 1,000 residents (this excludes resort open spaces, such as golf courses). Using the Quimby Act's, California Government Code Section 66477, standard of 3 acres per 1,000 residents, the City provides approximately 40% of the recommended park acreage (*107 acres of parkspace with a population of 87,883 in 2018*). Key strategies moving forward will include expanding parkland citywide and targeting specific neighborhoods in an effort to reach disadvantaged populations.

The City maintains sixteen existing parks, and owns several other properties which may be developed as parks in the future. Indio is also exploring opportunities to provide a network of recreational trails for residents and visitors to enjoy. Existing pedestrian hiking trails are located generally northeast of Golf Center Parkway, as well as along Avenue 38 adjacent to the Talavera/Andreas Ranch development. The following parks improvements and construction efforts are expected:

- ✦ Miles Avenue Park rehabilitation;
- ✦ Expansion of Dr. Carreon Park;
- ✦ Development of a sports park located south of Avenue 44 and north of Market Street; and
- ✦ Development a neighborhood park located south of Avenue 49 and west of Monroe Street.

The following trail expansion projects are also planned:

- ✦ Development of CV Link and trail spurs;
- ✦ The North Indio Trailhead located at Golf Center Parkway and Ave 42;
- ✦ Internal pedestrian trails and trailheads connecting to the Bureau of Land Management (BLM) lands in and surrounding the Virada Specific Plan area;
- ✦ Hiking and pedestrian trails within the Indio Trails Specific Plan area;
- ✦ Hiking and pedestrian trails within the Citrus Ranch Specific Plan area; and
- ✦ Recreational trails within the Indio Ranchos Polo Resort Specific Plan area and Indio Ranchos Polo Estates Specific Plan area.

Additional opportunities to increase parkland include negotiating joint-use agreements with the school districts to ensure school recreational facilities are open to the public during non-school hours, and

working with utility and public service providers to allow recreational easements along utility corridors. Another potential opportunity is allowing vacant property to be leased for the establishment of temporary urban parks or community gardens to create a sense of place and promote economic development.

The Coachella Valley Association of Governments (CVAG) is currently working on the Coachella Valley Link (CV Link), a multi-modal transportation route that is intended to traverse the Coachella Valley. CV Link serves as an integral component of Indio’s recreational open space system by accommodating cyclists, pedestrians, low-speed electric, and other alternative-powered vehicles. Accompanying recreational and commercial uses will be permitted along this route, such as healthy food stands, bicycle rental stations, and charging stations. As described in the Mobility Element, the City is designing a multi-modal transportation network that will provide connections to the CV Link and other recreational open space areas throughout Indio.

Context

Existing Parks and Recreation Facilities

There are sixteen existing City parks totaling 107 acres distributed throughout Indio. Each park provides a variety of amenities that support passive and active recreation. Table 7-1 summarizes the existing public parks in Indio. Figure 7-1 identifies the location of existing public parks in Indio, as well as their respective service areas.

Table 7-1: Existing Park and Recreation Facilities

Park Name	Size	Park Type	Amenities
South Jackson Park and Davis Field	17.5 acres	Community	Soccer field, racquetball, tennis courts, concessions with restrooms, open turf, picnic grounds, playground equipment, public art, event space, and group shelter. It also includes the Pawley Pool Family Aquatic Complex, owned by the Desert Recreation District. This complex features a lap pool, wading pool, and splash pad
Yucca Park	1 acre	Neighborhood	Water play, basketball courts, picnic grounds, playground equipment, and group shelter
Dominguez Park	3.3 acres	Neighborhood	Water play, a basketball court, loop walk, open turf, picnic grounds
North Jackson Park and Rotary Field	4.5 acres	Community	Water play, softball fields, basketball court, racquetball, tennis court, concessions with restrooms, loop walk, picnic grounds, playground equipment, and group shelter
Miles Avenue Park	16 acres	Community	Water play, basketball court, open turf, picnic grounds, playground equipment, restroom, event space, and group shelter
Shields Park	1 acre	Mini	Loop walk and picnic grounds
Cahuilla Park	4 acres	Neighborhood	Water play, basketball court, open turf, picnic grounds, playground equipment, group shelter

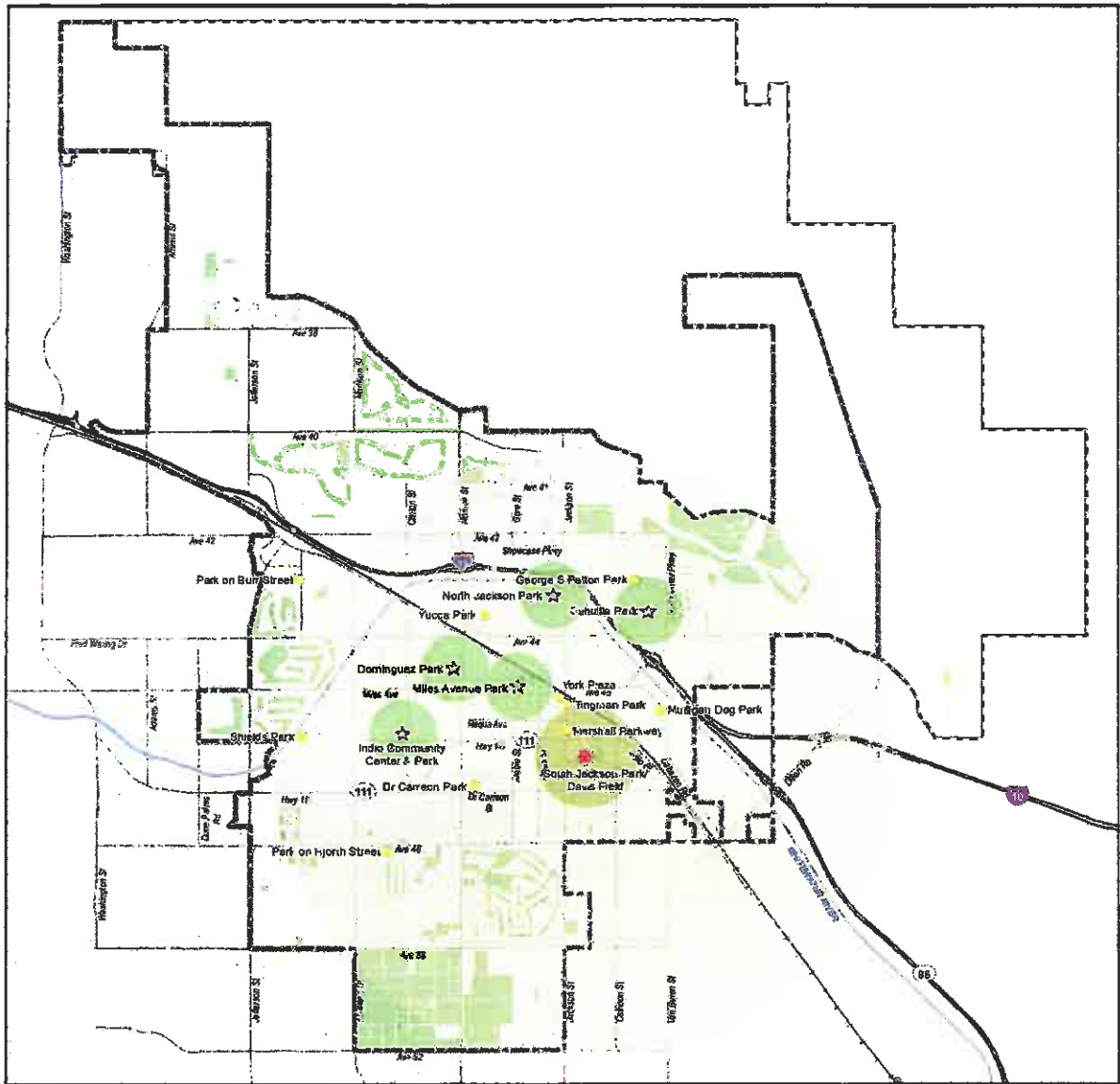
George S. Patton Park	4.3 acres	Neighborhood	Basketball court, tennis court, open turf, playground equipment, restrooms, and group shelter
Dr. Carreon Park	2.4 acres	Neighborhood	Water play, basketball court, loop walk, open turf, playground equipment, public art, restrooms
Doug York Plaza	0.25 acre	Mini	Open turf and group shelter
Mulligan Dog Park	2 acres	Neighborhood	Dog park with fenced dog run and benches
Marshall Parkway	0.5 acre	Mini	Garden and playground equipment
Burr Street Park	2.5 acres	Mini	Basketball court, playground equipment, picnic grounds
Hjorth Street Park	3 acres	Neighborhood	Loop walk, open turf, picnic grounds
Municipal Golf Course	46 acres	Special Use	Driving range, clubhouse/pro shop, putting green, 3 par course
Station 87 Dog Park	0.8 acres	Mini	Dog park with fenced dog run, separation for small and large dogs, benches, water and shade structure

The South Jackson Community Park also hosts the Boys and Girls Club John Carroll and Jim O’Brien Clubhouse. This clubhouse serves as an afterschool center for local students. In addition to these parks, the Desert Recreation District operates the Indio Community Center, gymnasium, and park located at Avenue 46 and Clinton Street. The Community Center offers a variety of programs and services to residents including fitness programs, children’s programs, and recreation opportunities, which are enhanced by the outdoor park space. The park includes playground equipment, a walking path and benches, and open turf areas.

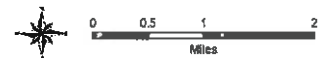


South Jackson Park

Figure 7-1: Existing Parks and Recreation Facilities



Existing Park Service Radii



- City Boundary
- Sphere of Influence
- Roads
- Railroads
- Highways
- Other Open Space / Recreation
- Parks Under 5 Acres
- Neighborhood Parks 5-20 Acres
- Community Parks 20-100 Acres
- Mini Parks: 1/4 Mile Radius
- Neighborhood Parks: 1/3 Mile Radius
- Community Parks: 1/2 Mile Radius
- Community Parks: 3 Mile Radius

Source: Riverside County LAFCO (City Boundary, 2012; 501,2012); Riverside County TMA (Roads, Railroads, Highways); Riverside County (Other)

The information on this map was derived from various digital databases sourced above. Care was taken in the creation of this map but it is provided "as is". TDC cannot accept any responsibility for any errors, omissions, or personal liability, and therefore, there are no warranties which accompany this product. Users are cautioned to thoroughly review information on this product before making any decisions.

Parkland Classifications

As the City continues to acquire and develop parkland, the following classifications serve as a guideline for identifying the size and amenities to be provided for various types of parks. Refer to Figure 7-1 for a map of existing parks and recreational facilities.

Mini Parks

Parks that exist primarily in residential areas and have features, such as grass, shade trees, passive areas, green space, shelters, native plant life, playgrounds, and play structures. They may be implemented in “tot-lot” configurations that provide secure recreational space for small children and include equipment, such as swings, slides, ‘monkey bars’, and sand boxes, while leaving sufficient room for people to sit and enjoy the space. Mini-parks are typically less than an acre in size and are intended to serve a population within walking distance or short biking distance. Indio’s mini parks include: Shields Park, Doug York Plaza, Marshall Parkway, Burr Street Park, and Station 87 Dog Park.



Station 87 Dog Park

Plazas and Greens

Typically, two acres or less, plazas and greens host a wide variety of activities that serve as formal or informal community gathering spaces. These park types are shaped primarily by building frontages and are primarily hardscape with occasional landscape in planters or containers. Greens are landscaped open areas located at the center of a community. Features may include gazebos, water features, trees, shade, performance areas, public artwork, and other similar features.



Example of a typical plaza

Neighborhood Parks

Neighborhood parks provide unobstructed areas for passive or active recreation and are primarily located in residential areas. They include features such as: grass, trees, restrooms, tot lots, picnic and shade shelters, grills, playground equipment, open fields, informal sports areas, swimming pools, community gardens, or neighborhood centers. Neighborhood parks typically serve the surrounding neighborhood and are less than ten acres in size. Cahilla and Dr. Carreon Parks are examples of neighborhood parks in Indio.



Dominguez Park

Community Parks

Community parks are designed to serve all residents of the City. They are typically 10 to 50 acres in size and include features such as: a swimming pool, a community center, sports areas or complexes, lights, entertainment areas, large grass areas, large picnic and shelter areas, restrooms, on-site parking, special features such as skateboard areas, outdoor theatres, disc golf, BMX, exercise station, ponds, or water features. South Jackson Park is an example of a community park.



South Jackson Park

Special Use Areas

Special use areas may include a variety of activities including: large-scale sports complexes, fair grounds, polo grounds, tennis courts, swimming pools, gardens, concessions, lakes, and animal uses. Special use areas serve the entire community and may serve adjacent cities. The Fairgrounds and Empire Polo fields are examples of a special use area in Indio.



Empire Polo Fields

Open Space

Areas generally free from development or developed with low-intensity uses that respect the natural environment. Open space areas and serve as preservation of natural, cultural, archaeological resources, passive outdoor recreation, public safety and health, and shape urban form. This includes areas such as the Indio Hills and Whitewater River.



Open space in the Coachella Valley

Greenways and Trails

Greenways and trails consist of walking, biking, hiking, equestrian, greenway, long distance, off road, rail, canal, and waterway paths with the purpose to meet passive or active recreation, informal, or formal transportation. CV Link is an example of a greenway.



CV Link

Urban Agriculture

Urban agriculture also provides many benefits to public health, environmental sustainability, and economic vitality. Locally-grown food ensures healthy, nutritious, and fresh foods are accessible to the public. Urban agriculture also reduces the environmental impact of transporting food long distances to reach the consumer (i.e. vehicle miles traveled and greenhouse gas emissions) and can provide social, physical, and emotional benefits of gardening.

In order to continue to provide access to locally-grown food, the City is creating opportunities to integrate small-scale agricultural activities into the City's neighborhoods in publicly-accessible community gardens, and edible landscaping on City property. Schools can also take part in providing urban agricultural activities by planting gardens on school properties that can serve as an educational resource and a healthy food source.

Goals and Policies

Open Space

A comprehensive network of parks and open space with both active and passive recreational facilities, promote healthy living and community wellness, and enhance the character of Indio.

Goal PR-1: Open Space Network: A comprehensive open space network that balances recreation, wellness, and habitat protection.

PR-1 Policies

PR-1.1 Accessibility to Open Space. Ensure that areas designated as open space for public use remain accessible to users of all ages and abilities.

PR-1.2 Balance Conservation Efforts with Accessible Open Space. Balance public access to natural open space resources with habitat and wildlife conservation efforts by locating trail easements within the less environmentally-sensitive areas and directing users to remain on designated trails through signage or fencing. Implement the Coachella Valley Multi-Species Habitat Conservation Plan.

PR-1.3 CV Link. Support development and maintenance of the CV Link along the Whitewater River as a fundamental and integral part of Indio's active recreational open space system, along with supportive, ancillary uses (e.g. healthy food stands, bicycle and alternative-fuel vehicle rental stations, and similar uses).

PR-1.4 CV Link Spurs. Implement development of "spurs" that connect CV link to neighborhoods along Avenue 48 and Madison Street, and in North Indio.

PR-1.5 Trail Connections. Encourage projects to provide connections to existing trails, and include mini parks, dog parks, community gardens, or other recreational features within the design.

PR-1.6 Regional Open Space. Support State and regional efforts to create regional open space networks.

Parks

The City offers parkland for existing and future residents, employees, and visitors.

Goal PR-2: High-Quality Parks. High-quality parks and recreational facilities that promote community health and are safe and convenient to access.

PR-2 Policies

PR-2.1 Park Ratio. Work to achieve a ratio of 3 acres of public parks per 1,000 residents and place a park or recreation amenity within one-half mile of all Indio residents. Maintain adequate land dedication requirements and/or development fees for new development to meet the park ratio.

PR-2.2 Parks classification system. Utilize the parks classification system identified in the General Plan to help guide public park planning and improvements in the City.

PR-2.3 Infill Parks in Disadvantaged Neighborhoods. Focus City efforts to create a variety of new parks types, including mini, neighborhood, and community gardens within Indio's existing

neighborhoods, particularly the most disadvantaged neighborhoods as identified in the Health and Equity Element. Ensure parks are safe, easy to access, and well maintained.

PR-2.4 Temporary / Pop-Up Parks. Encourage the establishment of temporary or pop-up urban parks, public plazas, community gardens, fitness opportunities, or similar on vacant land, parking lots, or undeveloped land until permanent development occurs. Use as a short-term placemaking strategy in Downtown, Midtown, and other areas of the City.

PR-2.5 Design for Heat. Design cool parks and playgrounds to facilitate activity in hot weather conditions by including shade structures, shade trees, water fountains, splash pads, lighting for night play, and other design features that mitigate heat.

PR-2.6 Design for Safety. Utilize CPTED design techniques, such as providing clear lines of sight, adequate lighting, and wayfinding signs, to ensure parks are safe. Face residential and commercial buildings towards new parks.

PR-2.8 New Development. Ensure that new residential developments provide adequate on-site recreational and open space amenities consistent with the values and standards of the community and the needs of new development. Require projects to establish mechanisms, such as a Community Facilities District, to adequately maintain new parks and recreational facilities.

PR-2.9 Existing Gated Communities. Work with developers of residential neighborhoods to allow the general public access to private open space and recreational spaces.

PR-2.10 Maintain Public Parks. Provide the appropriate resources and staffing to improve and maintain existing park and recreational facilities in Indio.

PR-2.11 Art in Public Spaces. Ensure new park facilities have adequate spaces and equipment for active and passive recreation, as well as incorporate public art into the park design.

PR-2.12 Recreation Programs. Promote opportunities for physical activities for all ages and abilities by improving and expanding community recreation programs.

PR-2.13 Partner with Local Schools. Seek opportunities to partner with local schools and public agencies to allow for the joint use of school properties as parks and recreational centers that are open to the public during non-school hours, and to develop community gardens on school property.

PR-2.14 Park Facility Locations. Ensure that public facilities and services are equitably distributed throughout Indio.

Public Trails

Locate and design trails to protect sensitive habitats and natural resources to the extent practical.

Goal PR-3: Quality Trails Network. Trails sited to ensure compatibility with natural resource protection and to encourage physical activity.

PR-3 Policies

PR-3.1: Trails Network. Support a comprehensive and cohesive system of recreational trails with linkages to the CV Link, key centers, parks, recreation, and open space areas.

PR-3.2: Trail Design. Design and manage trails in natural open space areas in a manner consistent with the Coachella Valley Multiple Species Habitat Conservation Plan. Coordinate efforts with local hiking and equestrian trail groups.

PR-3.3 Dedicate Space for Trails. Require new development projects to dedicate easements for trails, trailheads, and other needed improvements, where appropriate. Dedications may include paseos, urban trails, greenways, and/or Class I bicycle facilities that connect to centers, schools, parks, and open space areas. Seek opportunities to enhance them with informational kiosks, public art, outdoor fitness equipment, and rest areas.

PR-3.4 Utility Easements. Work with utility providers other key organizations and agencies to utilize utility easements for recreational purposes.

PR-3.5 Maintain Public Trails. Ensure adequate funding and resources are available to maintain the City's recreational trails.

Recreation

Indio has a rich history of festivals, special events, and community recreation. Continued support for these events will maintain the culture and character of the City.

Goal PR-4: Community Recreation. Offer a variety of recreational programs, festivals, and other special events that provide opportunities for community gathering and social interaction.

PR-4 Policies

PR-4.1: Equitable Access to Recreation and Special Events. Continue to ensure that residents have equal access and opportunity to participate in the City's recreational activities and programs regardless of age, economic status, disability, culture, or other factors

PR-4.2: Special Events. Provide opportunities for special activities, such as open mic nights, movie nights, community dances, plays, concerts, and similar events, within the City's parks and other appropriate public spaces for all ages and abilities.

PR-4.3: Temporary Open-Air Markets. Encourage the establishment of temporary markets and structures, such as farmers' markets, craft fairs, food trucks, and similar temporary or mobile vending opportunities. Use as a short-term placemaking strategy in Downtown, Midtown, and other areas of the City.

PR-4.4: Affordable Events. Seek opportunities to partner with the Indio Performing Arts Center, College of the Desert, County of Riverside Indio Library, Coachella Valley History Museum, and other cultural institutions to provide affordable events and entertainment.

PR-4.5: Youth Programs. Continue to support the Indio Teen Center, Boys and Girls Club, Desert Recreation District, after school programs, and other facilities and activities that cater to Indio's youth.

PR-4.6: Adult and Senior Programs. Continue to support the Senior Center/Mature Adults Center, Life Long Learning Programs, and other facilities and activities that cater to Indio's older adults.

PR-4.7: Promote Physical Activity. Promote physical activity by encouraging community sports and tournament events to take place within the City's parks.

Urban Agriculture

Urban agriculture, including community gardens, edible landscapes, and community-support agriculture, supports a local-food system that increases access to healthy and nutritious foods, encourages social connectivity, and reduces natural resource consumption for the production and distribution of food.

Goal PR-5: Urban Agriculture. Promote urban agriculture that supports public health, fosters community involvement, and provides a local food source.

PR-5 Policies

PR-5.1: Urban Agriculture. Support small-scale, urban agriculture and farming in residential areas, where possible, while ensuring such activities do not result in negative impacts on neighboring properties.

PR-5.2: Community Gardens. Establish an Indio community garden program that provides opportunities for residents and school children to grow fruits, vegetables, and herbs for personal consumption. Ensure that public community gardens and other urban agricultural resources are accessible to all members of the community.

PR-5.3: Underutilized Space. Allow underutilized park areas, utility right-of-ways, and other undeveloped land to be used as productive space for growing food.

PR-5.4: Farmers' Markets. Actively seek a farmers' market for Indio.

PR-5.5: Farm Stands. Allow farm stands diverse locations throughout Indio, including on commercial or mixed-use zoning districts, the Riverside County Fairgrounds, vacant lots, and parking lots.

PR-5.6: Edible Landscaping. Use edible gardens and landscaping, where appropriate, in residential, commercial, and public and institutional projects.

PR-5.7: Promote Healthy Food Education. Support educational opportunities for youth and adults to participate in healthy food activities, including community gardens, cooking classes, organic gardening, and health and nutrition classes.

PR-5.8: School Gardens. Work with the Desert Sands Unified School District and other local schools to develop community gardens on school sites to raise awareness and promote an urban food system.

Implementation Actions

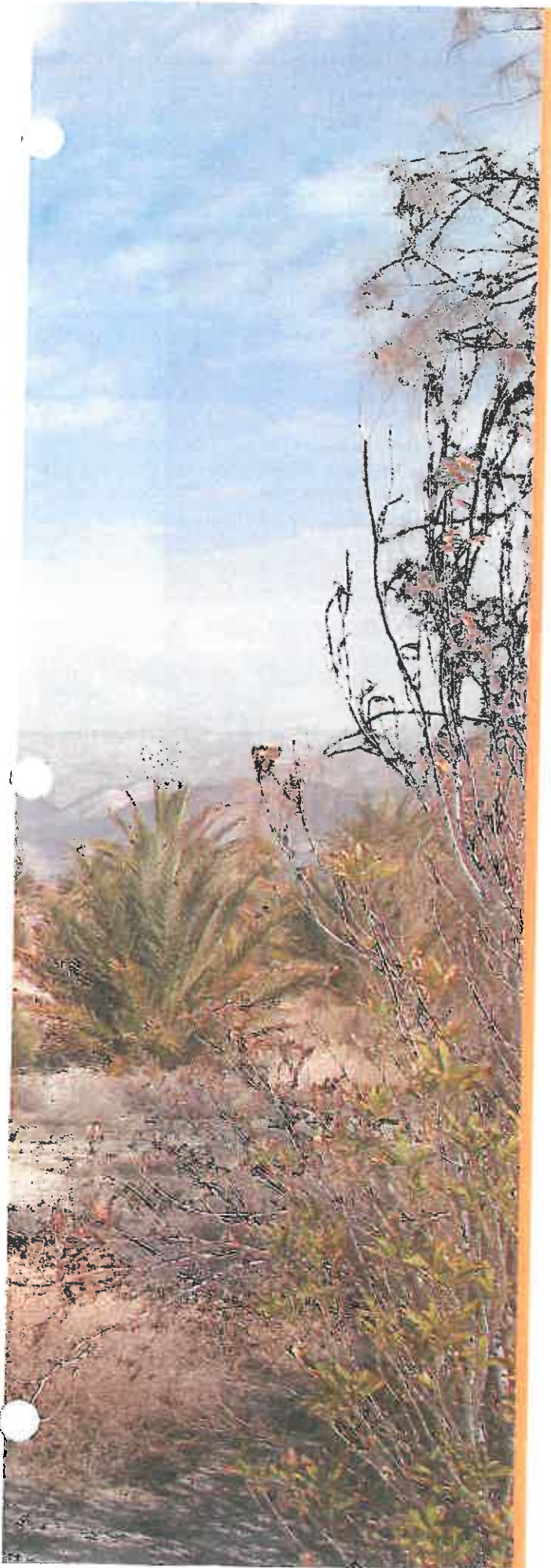
The table below identifies programs, policy updates, planning efforts, coordination efforts, and other actions that will help implement the General Plan’s parks, recreation, and open space vision and policies. Programs are consistent with this chapter’s goals and policies.

Table 7-2: Parks, Recreation, and Open Space Programs

	DESCRIPTION	PRIORITY	TIME FRAME	RESPONSIBILITY
1	<p>Park Master Plan. Prepare a Park Master Plan that includes the following:</p> <ul style="list-style-type: none"> • A comprehensive inventory of the City’s existing and planned parks and recreational facilities, including regional park and open space initiatives. • A comprehensive inventory of the City’s existing and planned trails. • An assessment of future park and recreational needs. • Preferred planning and zoning tools for new development, such as land dedication and in lieu fees. • Park funding and financing options, including mechanisms to maintain existing and future park resources, including development extract, general fund, bonds, and grants. 	High	Short	Planning, Community Services
2	<p>Park and Recreational Facility Fee Study. Conduct a park and recreational facility fee study, in conjunction with other fee studies defined in the General Plan, to support updated park and recreational impact fees.</p>	High	Short	Planning, Community Services
3	<p>Urban Agriculture. Amend the Municipal Code to implement the General Plan Update to support urban agriculture. Adopt regulations that specify the requirements for community gardens and other urban agricultural uses, such as farm stands, farmers’ markets, and animal keeping.</p>	Medium	Short	Planning
4	<p>Community Garden Program. Establish a Community Garden Program in the City.</p>	Medium	Medium	Community Services, Planning

5	Park Placemaking. Create incentives for converting vacant, underutilized, or parking lots into small parks or open spaces throughout the City.	Medium	Short	Planning
6	Increase Parkland. Establish a funding source to increase parkland within Indio by transforming abandoned utility corridors into linear parks, reclaiming unused street rights-of-way, or purchasing fallow farmland.	Medium	Short	Planning and Public Works
7	Trail Signage. Develop trail signage program to include signage along trails for interpretative purposes, and to display rules of appropriate use and conduct on trails.	Low	Short	Public Works
8	Edible Landscaping Program. Develop a program that permits planting fruit trees within parks or on other City property. Coordinate with non-profit organizations or community volunteers to identify funding sources, plant and maintain the trees, and harvest the fruit.	Low	Short	Planning and Public works, Parks
9	Joint Use Agreements. Create joint-use agreements with Desert Sands Unified School District to maximize shared community use of school facilities and City park facilities, where appropriate, to expand opportunities for physical activity.	High	Short	Planning, Community Services





chapter eight

CONSERVATION

The purpose of the Conservation Element is to address the conservation, development, and sustainable use of Indio's natural resources, including, but not limited to, water, soils, natural gas, fossil fuels, renewable energy sources, and mineral deposits. Strategies to improve air quality and reduce greenhouse gas emissions that contribute to global climate change are also discussed. In addition, this element provides guidance to enhance and protect cultural, paleontological, and historic resources as well as sensitive biological resources, such as the native desert plants and wildlife species that occupy the northern portion of the planning area.

Key Considerations and Strategies

Human population growth and urbanization is a worldwide concern. Human activities such as the burning of fossil fuels and deforestation are creating concentrations of greenhouse gases in the atmosphere. High levels of greenhouse gases contribute to higher global temperatures, resulting in changes to snow and rainfall patterns, glacial ice coverage, and sea levels. Such changes can have significant impacts on the availability of water and energy resources at the local level. In addition, climate change can have a negative impact on human health due to increased air pollution and heat-related illnesses.

To combat these problems, Indio is taking a leadership role in addressing sustainability, climate change, and resource conservation at the local level. In 2017, the City of Indio was awarded the gold level Beacon Award from the Institute for Local Government. This award recognized the accomplishments of Indio, including:



Drought-tolerant landscaping

- + Purchasing City electric vehicles and installing solar electric vehicle charging stations;
- + Expediting the review and permit process for solar projects;
- + Reducing greenhouse gas emissions by using local vendors and locally-produced goods;
- + Replacing grass with drought-tolerant landscaping throughout the City;
- + Enacting a recycling ordinance requiring 65% or more diversion of project waste; and
- + Participating in regional and local planning efforts to offset carbon emission.

This General Plan builds on those existing efforts by including a broad policy framework to reduce greenhouse gas emissions and adapt to climate change. For example, the Land Use and Community Design Element focuses new growth and redevelopment into compact mixed-use areas, such as Midtown and Downtown, that provide opportunities for people to who live in adjacent connected neighborhoods to use public transit, walk, and bike. Along with multi-modal transportation improvements, this helps to reduce vehicle miles traveled and lower greenhouse gas emissions associated with driving. However, vehicles are not the only sources of greenhouse gas emissions. Residential buildings, commercial and industrial buildings, municipal buildings, solid waste, and water and wastewater systems also emit greenhouse gas and contribute to climate change.

Reducing the impact of these human activities on the natural environment is a priority for Indio. This Conservation Element addresses topics, such as green building code regulations, renewable energy sources, energy and water conservation measures, low impact development, solid waste reduction, and public awareness programs that contribute to the long-term health and viability of Indio's natural resources.

Consistent with General Plan goals and policies, the City of Indio has developed a Climate Action Plan that sets forth a series of strategies to achieve greenhouse gas reduction targets. These efforts are in line with the number of laws and regulations that California has passed to address climate change, such as the Senate Bill 32, which requires greenhouse gas emissions to reach 40% below 1990 levels by 2030.

Land use development pressure can also impact areas that support natural resources and that help define Indio's identity, contribute to community health, and are relied upon for economic prosperity. This General Plan supports objectives to limit development in these areas. For example, the northern portion of the planning area (refer to Figure 8-4) encompasses a diverse landscape of desert and hillsides that form the scenic backdrop to the City and support many sensitive plant and animal species. These species contribute to biological diversity and are crucial to maintaining a healthy ecosystem. Many of these species are protected under the federal Endangered Species Act (ESA). To safeguard these areas, the City of Indio complies with the Coachella Valley Multiple Species Habitat Conservation Plan (CVMSHCP), which balances environmental protection and economic development objectives.

The northern portion of the planning area also contains important mineral resources, such as sand and gravel, which provide necessary materials for the local economy (refer to Figure 8-2). This Element promotes the long-term viability of mineral resource sites.

Indio has, and will continue to, celebrate and embrace its past. The preservation of significant cultural and historic resources serves to strengthen community identity and provides educational opportunities. This Conservation Element promotes the preservation and maintenance of these resources.

Context

Water Conservation

Both the Indio Water Authority (IWA) and the Coachella Valley Water District (CVWD) have implemented water conservation programs to reduce water demand. Water conservation efforts include such measures as a Landscaping and Water Conservation Ordinance, a hotline to report water misuse, rebate programs, a Water Conservation Master Plan, education and outreach programs, landscape audits, water-wise landscape workshops and seminars, and an agricultural conservation program.



Water waste

Energy Conservation

The City of Indio has made significant progress towards conserving energy. As part of the Desert Cities Energy Partnership (DECP), the City has received direction and support in identifying and addressing energy efficiency and demand response opportunities for both municipal facilities and the broader community. The City has sponsored and supported a wide variety of environmental and sustainability programs including:

- + Energy Star promotions and rebates;
- + Standard appliance efficiency improvements;

- ✦ Solar installations by the Imperial Irrigation District (IID), the Indio Water Authority (IWA), and City Hall;
- ✦ Solar installation at the City Corporate Yard;
- ✦ Solid waste recycling and diversion programs;
- ✦ Water saving initiatives;
- ✦ Fleet fuel shifting from gasoline to compressed natural gas (CNG) by SunLine, Burrtec, and Desert Sands Unified School District;
- ✦ The use of alternative-fuel vehicles in the municipal fleet service; and
- ✦ The adoption of a plastic bag ban.
- ✦ School outreach programs
- ✦ Sustainability Commission

Additionally, the Southern California Gas Company (SCG) is responsible for providing natural gas services to Indio. SCG provides rebate and incentive programs that reward residents for using energy efficient natural gas equipment. Residential customers are eligible for rebates if they install high-efficiency water heaters, clothes washers, and furnaces, utilize low-flow showerheads, or insulate their attics and walls. In addition, customers can receive incentives for making other energy efficient-upgrades to their homes.

Urban Forest

Indio recognizes the importance of preserving and maintaining the urban forest. Indio is a member of Tree City USA, which is a nationwide movement that provides a framework for communities to manage and expand their public trees. Urban forests provide many environmental benefits and quality of life benefits, such as improving water quality by capturing stormwater runoff, reducing the urban heat island effect by providing shade, producing oxygen, and creating sound buffers.

Indio's urban forest is comprised of trees along public rights-of-ways, in parks, in parking lots, and along private and publicly-owned property. The urban forest is most prominent in older neighborhoods where trees have fully matured. The Public Works Department is committed to protecting existing trees, properly planting and maintaining new trees, and educating the public about the benefits of maintaining a healthy urban forest.

Greenhouse Gas Emissions

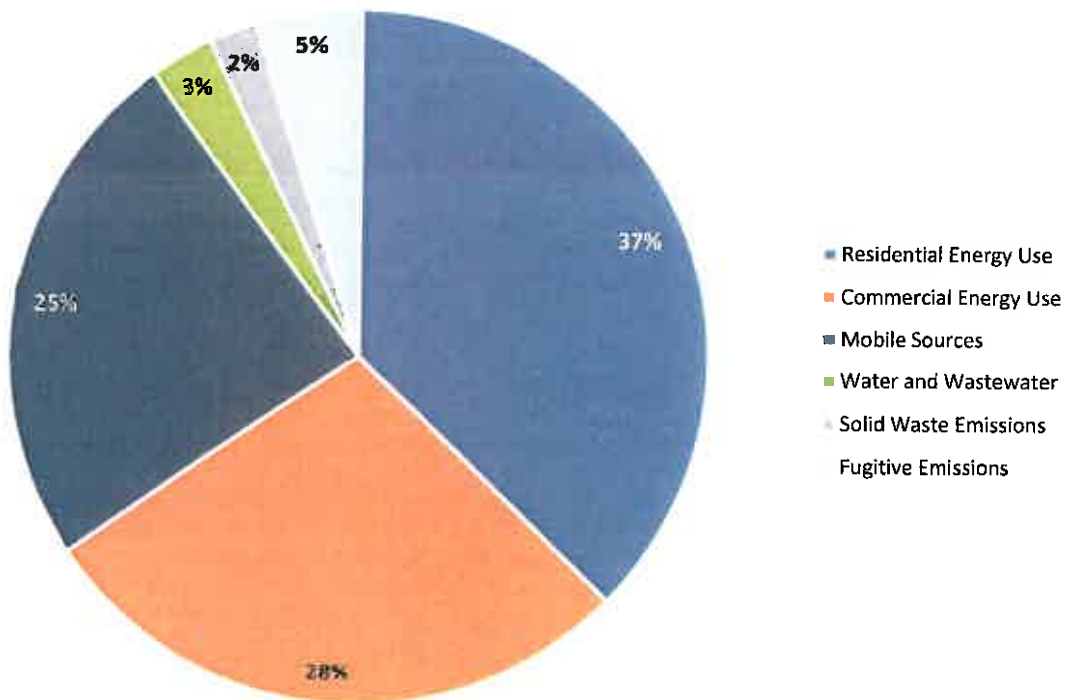
The Indio greenhouse gas emissions inventory provides a snapshot of emissions for 2010 to quantify the main sources of emissions from municipal operations and the community as a whole. The purpose of the inventory is to:

- ✦ Identify and understand the sources and quantities of emissions within a local government's jurisdictional boundary;
- ✦ Create an emissions baseline that can be used by the City to measure progress towards emissions reductions targets;

- + Use the baseline to prioritize and evaluate potential government actions; and
- + Make informed policy decisions related to greenhouse gas emissions reduction.

In 2010, total greenhouse gas emissions in Indio were approximately 610,000 metric tons of CO₂e (MTCO₂e). On a per person basis, this is approximately 8 MTCO₂e per Indio resident, less than the State average of 11.9 MTCO₂e. These emissions account for direct emissions from the on-site combustion of fuels and the combustion of fuel in vehicles, indirect emissions associated with electricity consumption, and emissions from solid waste generated and water consumed by Indio. Figure 8-1 shows communitywide greenhouse gas emissions by sector.

Figure 8-1: 2010 Communitywide Greenhouse Gas Emissions



Mineral Resources

Indio's important mineral resources include sand, gravel, and termed aggregate. These minerals are an important component of asphalt, concrete, road base, stucco, and plaster, and provide materials for the local economy.

The State Mining and Geology Board has defined Mineral Resource Zones for Indio, which describes mineral resources deposit areas (Table 8-1). The Indio Quarry/Indio Hills Fan, located within Indio's Sphere of Influence, is an existing permitted sand and gravel operation. The subject resource area consists of a moderate-sized deposit that is located within 750 acres of an alluvial fan adjacent to and immediately south of Indio Hills as shown in Figure 8-2.

Table 8-1: Description of Mineral Resource Zones

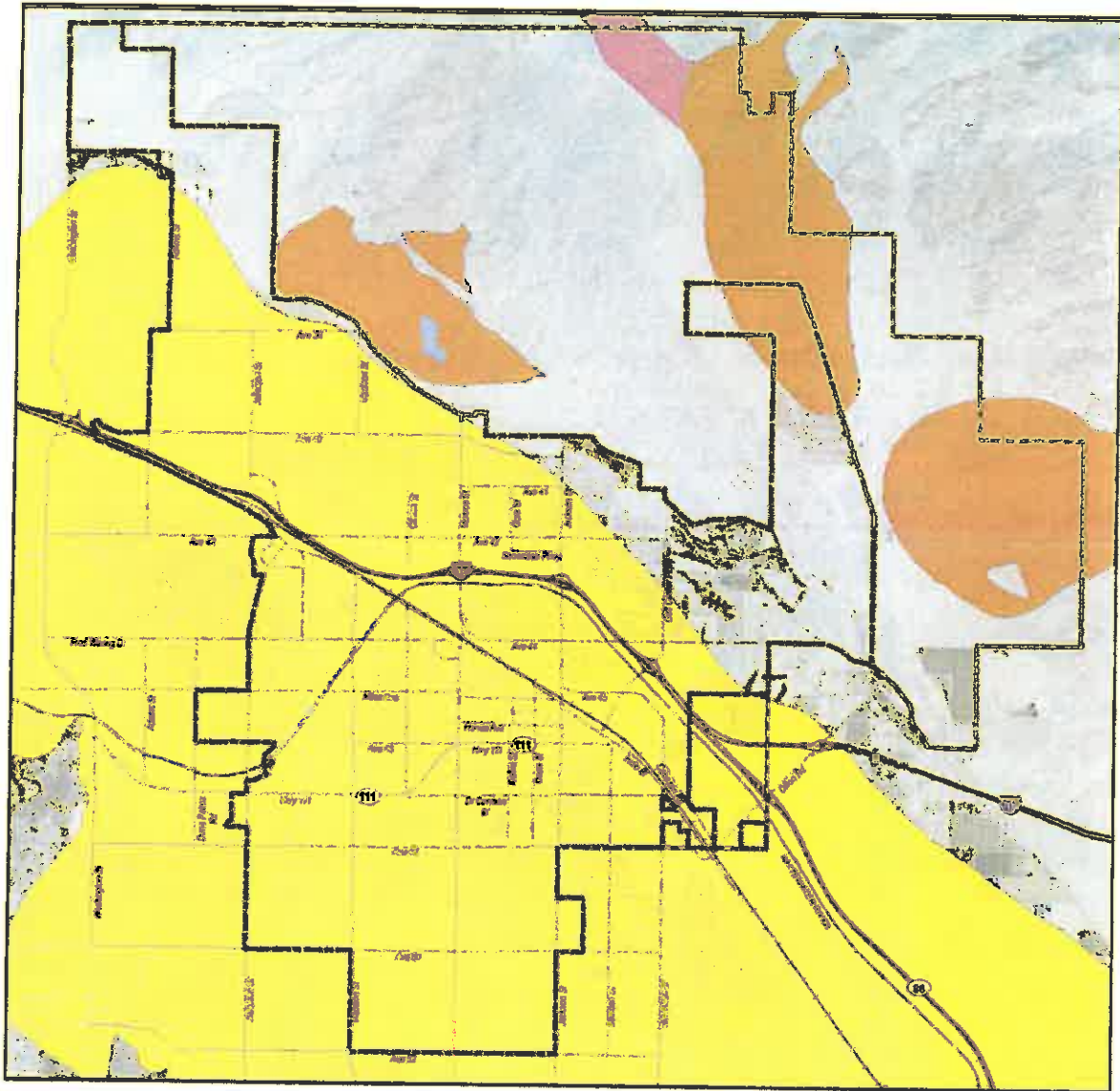
Zone	Description
MRZ-1	Areas where available geologic information indicates that little likelihood exists for the presence of significant mineral resources.
MRZ-2a	Areas underlain by mineral deposits where geologic data indicate that significant measured or indicated resources are present. Contains known economic mineral deposits.
MRZ-2b	Areas underlain by mineral deposits where geologic information indicates that significant inferred resources are present.
MRZ-3	Areas containing known or inferred mineral occurrences of undetermined mineral resource significance.
MRZ-4	Areas where available information is inadequate for assignment to any other MRZ.

Source: 2007 California Geological Survey. Special Report 198.






Soils

There is a wide variety of agricultural soils in Indio. Soils vary appreciably in origin, degree of weathering, and depth and texture. There are still many locations in Indio which have productive soil and other requisites to be especially well suited for agricultural purposes. Soils in the region with higher Storie Indices and capability classifications include the Coachella, Gilman, and Indio soil series, which compose approximately 57% of Indio's total area. Figure 8-3 identifies the soil types within the planning area.

Figure 8-2: Mineral Resource Zones in Indio



Mineral Resource Zones

-  City Boundary
-  Sphere of Influence
-  Roads
-  Railroads
-  Highways

Mineral Resource Zones (2008)

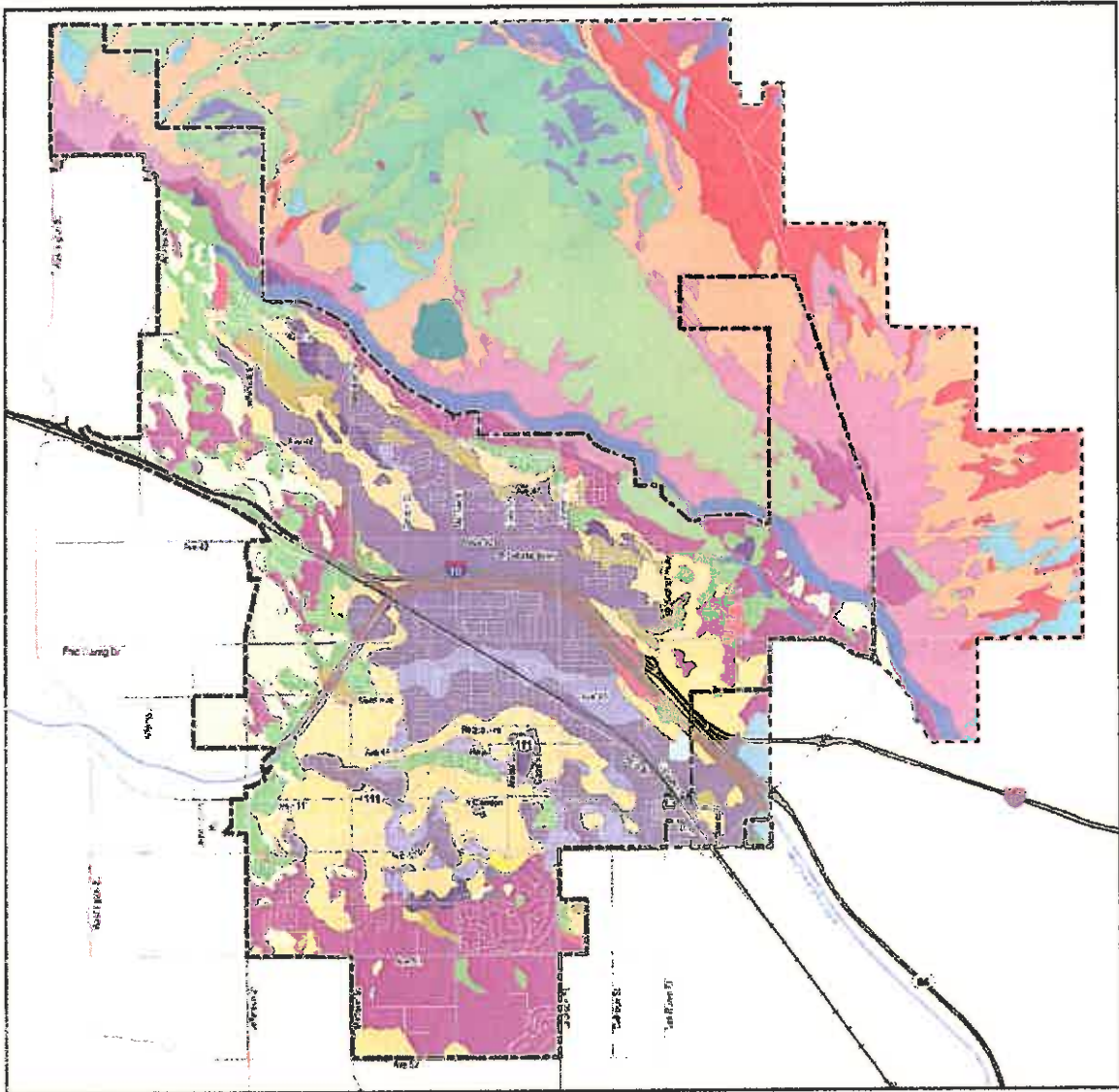
-  MRZ-1
-  MRZ-2a
-  MRZ-2b
-  MRZ-3



Source: Riverside County/LAFCD (City Boundary, 2010; SDX, 2012); Riverside County/TDMA (Roads, Railroads, Highways); Riverside County (Title)

The information on this map was derived from various digital databases sourced above. Care was taken in the creation of this map but it is provided "as is". PDC cannot accept any responsibility for any errors, omissions or professional negligence and liabilities that may arise from any use of this product. Users are cautioned to take very close attention to this product before making any decisions.

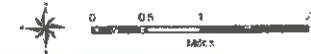
Figure 8-3: Soil Types in Indio



Soils

- City Boundary
- Sphere of Influence
- Roads
- Highways
- 1713 Goldenlake Buletpool and Whitecote complex, 30 to 75% slopes
- BA Bantland
- BP Borewicks
- COC Calico clay sand, 2 to 8% slopes
- COC, Calico gravelly sand, 0 to 5% slopes
- CNC, Calico rhyolite sand, 2 to 5% slopes
- CoB, Chuckawalla very gravelly sandy clay loam, 0 to 5% slopes

- CoD, Chuckawalla very gravelly sandy clay loam, 5 to 15% slopes
- CpA, Coachella fine sand, 0 to 2% slopes
- CpA, Coachella fine sandy loam, 0 to 2% slopes
- Fe, Fluvials
- GP, Gravel pits and dumps
- GAR, Gilman clay fine sand, 0 to 5% slopes
- GBA, Gilman fine sandy loam, 0 to 2% slopes
- GBB, Gilman fine sandy loam, 2 to 5% slopes
- GCA, Gilman fine sandy loam wet, 0 to 2% slopes
- GEA, Gilman silt loam, 0 to 2% slopes
- GBA, Gilman silt loam wet, 0 to 2% slopes
- IeA, Imperial silt clay, 0 to 2% slopes
- Ie, Indio fine sandy loam



- Ie, Indio fine sandy loam wet
 - Is, Indio very fine sandy loam
 - LR, Litch Turpentine-Rock outcrop complex
 - MAD, Myoma fine sand, 0 to 5% slopes
 - MAD, Myoma fine sand, 5 to 15% slopes
 - RU, Rubble fan
 - W, Water
- Source: Modified from the U.S. Department of the Interior, Bureau of Land Management, 1991. The map is a generalization of the soil data and does not represent the actual soil conditions. The map is for informational purposes only and should not be used for engineering or other purposes. The map is not a warranty of any kind. The map is provided as a service to the public and is subject to change without notice. The map is not a representation of the actual soil conditions. The map is for informational purposes only and should not be used for engineering or other purposes. The map is not a warranty of any kind. The map is provided as a service to the public and is subject to change without notice.

Biological Resources

The Coachella Valley Multiple Species Habitat Conservation Plan (CVMSHCP) provides a regional vision for balanced growth to meet the requirements of federal and State endangered species laws, while promoting enhanced opportunities for recreation, tourism, and job growth. The CVMSHCP aims to conserve over 240,000 acres of open space and protect 27 plant and animal species across the region. By providing comprehensive compliance with federal and State endangered species laws, the CVMSHCP not only safeguards the desert's natural heritage for future generations, it also allows for more timely construction of roads and other infrastructure that is essential to improving quality of life in the Coachella Valley. The Indio City Council adopted the plan in 2008, and it guides development within key conservation areas.

The CVMSHCP designates five conservation areas where development is limited to protect environmentally sensitive lands:

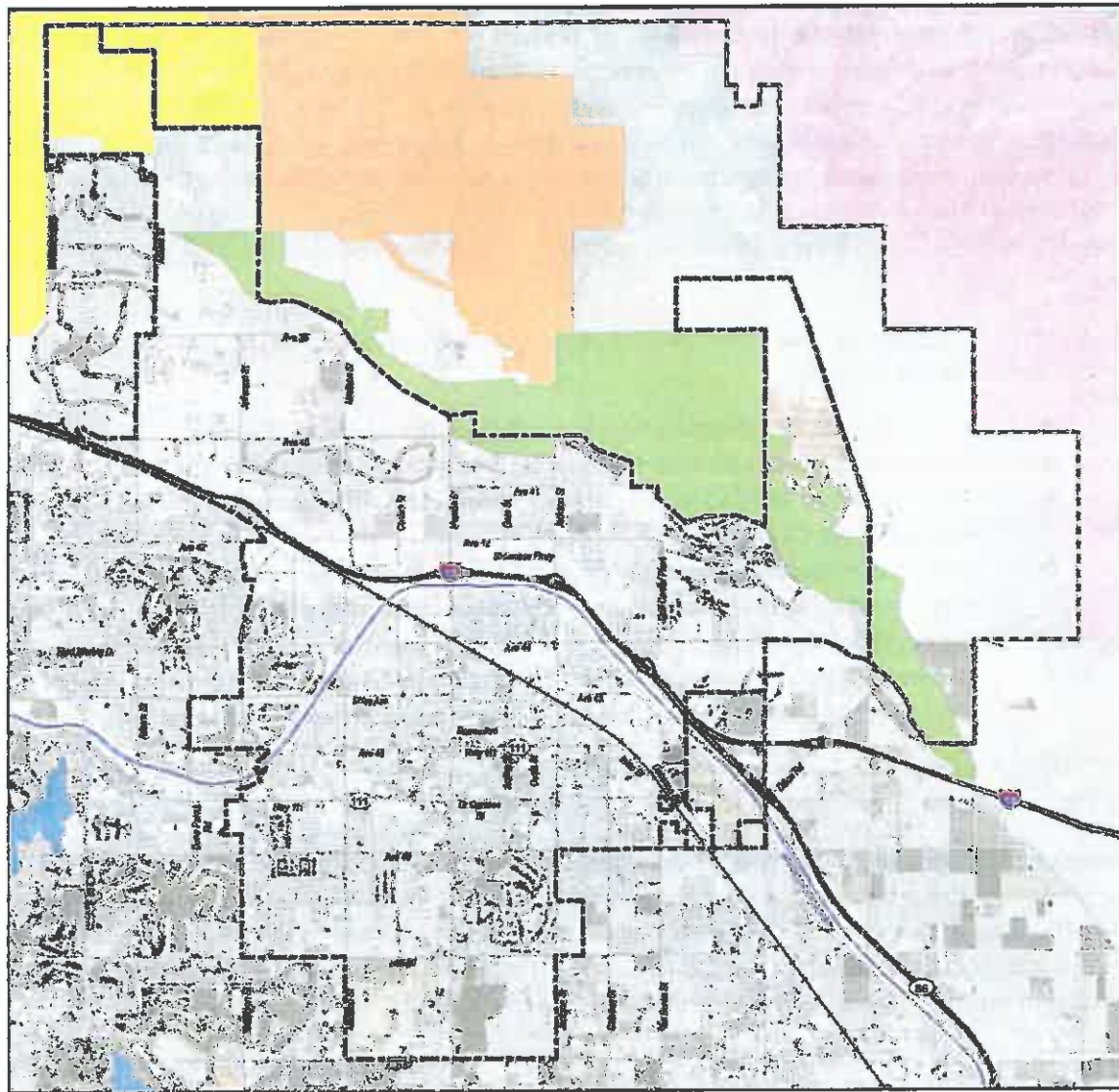
- + East Indio Hills Conservation Area located along the northern City limit;
- + Thousand Palms Conservation Area located near the northwest corner of the City limits;
- + Santa Rosa and San Jacinto Mountains Conservation Area located to the southwest and outside of the City limits;
- + Indio Hills Palms Conservation Area located along the northern edge of and outside of the City limit but within the sphere of influence area adjacent to the Thousand Palms Conservation Area; and
- + Desert Tortoise and Linkage Conservation Area located just outside the northeast part of the sphere of influence area with a small portion located within the sphere of influence.

The CVMSHCP is shown on Figure 8-4 and on the Land Use and Community Design Element Place Type map (Figure 3-5) as an overlay zone.

Although most of Indio has been largely developed, biological resource areas are present within the City urbanized area. These resources are confined to areas of native vegetation that occur in the northwest and northeast portions of the planning area, generally north of I-10 and lie partially within designated conservation areas that are part of the CVMSHCP area. These conservation areas provide core habitat for a number of sensitive plants, insects, mammals, birds, and reptiles.

The Coachella Valley and Indio is also part of the Whitewater River watershed which drains into the Salton Sea and is part of the larger Colorado River Watershed. The watershed is also home to a diverse desert landscape that provides habitat for dozens of plant and animal species.

Figure 8-4: CVMSHCP Areas in Indio



CVMSHCP Conservation Areas



- City Boundary
- Sphere of Influence
- Roads
- Railroads
- Highways
- Desert Tortoise and Linkage Conservation Area
- East India Hills Conservation Area
- Indio Hills Palms Conservation Area
- Santa Rosa and San Jacinto Mountains Conservation Area
- Thousand Palms Conservation Area

Source: Riverside County's ARCO | City Boundary (2012, SOI 2012)
 Riverside County's LMA | Roads, Railroads | Highways | Riverside County
 (2012)

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Cultural and Tribal Cultural Resources

Cultural Resources

Cultural resources include prehistoric and historic archaeological sites, buildings, structures, features (including significant trees or other landscaping), places, or other objects of historical, archaeological, scientific, educational, cultural, architectural, aesthetic, or traditional significance. Several prehistoric archaeological resource sites occur in Indio, including habitation sites, temporary camps, lithic and ceramic scatters, quarries, and trails. Archaeological sites in arid regions, such as Indio and the Coachella Valley, are more frequent in areas of permanent or seasonal water sources. Historic archaeological sites consist predominately of trash dumps/scatters but may also include structure remains.

Within Indio, archaeological resources will most likely be encountered in undeveloped parcels of land. However, subsurface archaeological resources may be encountered in areas that have been developed but have not been subject to extensive subsurface disturbance.

Historic Resources

Included under the broader umbrella of cultural resources is the subcategory of “historic resources,” also referred to as built environment resources. Historic resources can include buildings, structures, sites, objects, districts, and cultural landscapes that have architectural, cultural, social, or historic significance. In order to qualify as a historic resource, a property must meet applicable eligibility criteria and retain historic integrity.

A number of potential historic and prehistoric sites and structures have been recorded in the City. A number of these resources fall within the historic resource subcategory. In the case of Indio, these resources include a range of property types, such as residences, institutional, commercial, recreational, and agricultural properties. These resources reflect and embody the City’s rich cultural heritage, from its founding years as a small railroad town and date capital of the United States, through the war years and an extensive postwar building boom and City expansion.

In order to manage and preserve its cultural heritage, the City maintains a Historic Resource List. Established in 2008, the City of Indio Historic Resource List currently includes a total of 39 buildings that are eligible or potentially eligible as historic resources. An additional 19 properties are included on the City’s list as warranting evaluation. The list also includes all Unreinforced Masonry (URM) buildings in Indio.



Indio’s water tower at the Date Museum

Tribal Land

Three tribes are located within the vicinity of Indio. The Cabazon Tribal lands are located immediately east and southeast of the City. Augustine Tribal Lands and Torres-Martinez Tribal Lands are located to the south. Agua Caliente Tribal Lands are located to the northwest. The City engages with these tribes when planning development that may affect Native American cultural resources or sacred sites.

Paleontological Resources

Paleontological resources include the fossilized remains or traces of animals and plants from a previous geologic period. Indio is located in the Coachella Valley, a large northwest to southeast trending basin that is the result of the well-known San Andreas Fault system in California. Sedimentary deposition has been slowly filling this basin since the Miocene Epoch (23.0 to 5.3 million years ago). Being an area of sedimentary deposition, the potential for paleontological resources exists.

Five sedimentary units in Indio have the potential to contain significant paleontological resources:

- + The Mecca Formation, dated from the late Miocene/early Pliocene, for which little information is available, has been given a High Potential sensitivity ranking.
- + The Palm Springs Formation dates from the Pleistocene and contains vertebrate fossils such as horse and cotton rat. It is considered to have a High Potential sensitivity ranking.
- + The Canebrake Conglomerate, which dates from the Pliocene to the Pleistocene, has produced mammalian, reptilian, and avian fossils. It is also considered to have a High Potential sensitivity ranking.
- + The Older Quaternary Lake Sediments, found above the high shoreline of Lake Cahuilla, date from the Late Pleistocene and contain abundant fossil fresh water fauna. This unit is considered to have a High Potential sensitivity ranking.
- + The Ocotillo Conglomerate, an alluvial fan deposit, dates from the mid to late Pleistocene. It has been given a sensitivity rating of Undetermined Potential.

Goals and Policies

Greenhouse Gas Emissions

Indio seeks to meet greenhouse gas reduction targets set by the State. To achieve this, the City will continually evaluate total emissions produced by Indio and update strategies for reducing greenhouse gas emissions.

Goal CE-1: Greenhouse Gas Emissions. Indio maintains a leadership role in the Coachella Valley by reducing greenhouse gas emission to support a resilient community.

CE-1 Policies

- CE-2.1 Reduction Targets.** Establish greenhouse gas emission reduction targets in line with those of the State that call for reducing greenhouse gas emissions as follows:
- 1990 levels by 2020
 - 40 percent below 1990 levels by 2030
 - 60 percent below 1990 levels by 2040
 - 80 percent below 1990 levels by 2050
- CE-2.2 Reduction Measures.** Implement greenhouse gas reduction measures consistent with the Climate Action Plan to achieve greenhouse gas reduction targets.
- CE-2.3 Monitor Emissions.** Monitor and report greenhouse gas emissions so that reductions can be tracked in a transparent, consistent, and accurate manner.
- CE-2.4 Reduction Programs.** Use the emissions inventory and monitoring tools to identify, prioritize, and update programs that effectively contribute to greenhouse gas reductions.
- CE-2.5 Municipal Emissions.** Prioritize municipal policies and programs that reduce the City's carbon footprint, such as purchasing alternative fuel vehicles, pursuing solar installation, implementing green purchasing, and retrofitting existing buildings.
- CE-2.6 Green Jobs.** Promote greenhouse gas reduction measures that support local job training and placement in green industries focused on environmental sustainability, renewable energy, renewable-related technologies, and bioremediation.
- CE-2.7 Sustainable Communities Strategy.** Collaborate with regional and State partners to implement the Sustainable Communities Strategy to reduce greenhouse gas emissions, balance jobs and housing, and develop transportation systems that support all modes of circulation.

Water Conservation

In order to ensure that there is a sufficient supply of water to meet current and future needs, it is important to continue to conserve water and explore other ways to reduce the demand for potable water.

Goal CE-2: Water Conservation. Sustainable domestic water facilities and water conservation measures to effectively meet current and future demand.

CE-2 Policies

- CE-2.1 Rate schedule.** Work with the Indio Water Authority to continue to maintain a water conservation rate schedule intended to reduce water use.
- CE-2.2 Regional coordination.** Continue to work with CVWD and IWA to implement water conservation projects, including conducting water audits, disseminating information about water conservation measures and rebate programs, and participating in regional water planning efforts.
- CE-2.3 New development requirements.** Require new development projects to implement water conservation measures that are equivalent to or exceed CalGreen Tier One or other applicable standards in effect at the time of development.
- CE-2.4 Drought-Tolerant Landscaping.** Exceed State landscaping water efficiency standards by requiring the use of drought tolerant landscaping, minimizing the use of turf, and encouraging the retrofitting of existing irrigation systems
- CE-2.5 Water-efficient landscaping and appliances.** Encourage the retrofitting of existing water-intensive appliances and irrigation systems. Continue to disseminate information about the CVWD and IWA rebate programs.
- CE-2.6 Greywater systems.** Encourage new residential development to be constructed to allow for easy implementation of greywater systems that redirect water from washbasins, showers, and tubs for use in toilet flushing, irrigation, and other non-potable uses.
- CE-2.7 Reclaimed and Recycled Water Use.** Work with the Coachella Valley Water District (CVWD), Valley Sanitation District (VSD), and the Indio Water Authority (IWA) to use reclaimed and recycled water.

Energy Conservation and Renewables

Most energy is generated by the use of non-renewable resources such as fossil fuels. Fossil fuels contribute to air pollution and release greenhouse gases into the atmosphere. It is necessary to reduce reliance on fossil fuels and develop renewable alternative energy solutions, such as solar and wind power. These are clean forms of energy that improve the overall health and sustainability of the community.

Goal CE-3: Energy Efficiency, Conservation, and Renewables. Energy-efficient buildings and vehicles that use energy from renewable sources.

CE-3 Policies

- CE-3.1 Regional Energy Leadership.** Continue involvement with CVAG and the Desert Energy Cities Partnership and be a regional leader in energy conservation, efficiency, and renewables implementation. Seek opportunities to join a Community Choice Energy program.

- CE-3.2 Residential Energy Efficiency Education.** Maintain a designated City staff member to oversee an Energy Awareness Program that provides lectures, seminars, and training on green building.
- CE-3.3 Low Income Weatherization Assistance Program.** Partner with Imperial Irrigation District and SoCal Gas to promote existing programs that provide financial assistance to low-income households for weatherization improvements and heating, ventilation, and air conditioning (HVAC) tune-ups.
- CE-3.4 Energy Audits.** Require energy audits to be performed on residences prior to sale or transfer of title and provide prospective owners with recommendations for retrofit measures.
- CE-3.5 Commercial Benchmarking.** Promote commercial benchmarking using the Environmental Protection Agency’s ENERGY STAR Portfolio Manager or equivalent benchmarking tool.
- CE-3.6 Zero Net Energy Use.** Implement building design requirements to achieve zero net energy use for new residential development by 2020 and zero net energy use for new commercial development by 2030 consistent with the California Public Utilities Commission’s California Long Term Energy Efficiency Strategic Plan.
- CE-3.7 Solar Financing.** Promote installation of solar panels by continuing to support Indio’s Ygrene and HERO Programs and by distributing information on actual savings achieved by PV systems.
- CE-3.8 Building Energy Use.** Encourage the use of building placement, design, and construction techniques to limit energy consumption, reduce the heat island effect, increase renewable energy use, and maintain solar access.
- CE-3.9 Municipal Buildings.** Continue to take a leadership role in ensuring that municipal buildings are designed to be as sustainable and energy efficient as feasible by:
- Requiring new City buildings to achieve LEED Gold certification or an equivalent standard;
 - Retrofitting existing buildings with renewable energy infrastructure or updating energy efficient appliances and fixtures;
 - Implementing stringent water conservation measures;
 - Capturing and reusing rainwater to the extent feasible; and
 - Planting new native or low water use trees in conjunction with City-initiated projects to expand Indio’s urban forest, decrease demand for air conditioning, and reduce the heat island effect.
- CE-3.10 Municipal Vehicle Fleet Upgrades.** When replacing or adding new municipal vehicles, require the analysis of alternative-fuel vehicles along with gasoline-fueled vehicles. When appropriate and economically-feasible, preference the purchase of alternative-fuel vehicles over gasoline-fueled vehicles.
- CE-3.11 Grant Funding.** Seek grant funding to implement a “green building” demonstration project to promote awareness of available “green” technologies that work within Indio’s desert setting.
- CE-3.12 Alternative Energy.** Explore future solar and wind initiatives with IID. Facilitate the development of small-scale alternative energy infrastructure, such as windmills and solar fields.
- CE-3.13 Innovative Systems.** Identify opportunities to implement innovative infrastructure systems that utilize natural ecological processes.

CE-3.14 Sustainability Commission. Work with the Sustainability Commission to identify and develop energy efficiency and sustainability programs.

Urban Forest

Urban forests improve water quality by removing pollutants from water as it is absorbed into the ground, reduce energy use by providing shade, and mitigate climate change by taking carbon dioxide out of the atmosphere. Urban forests also result in better air quality by releasing oxygen as a byproduct of photosynthesis.

Goal CE-4: Urban Forest. A healthy and thriving urban forest that results in improved air and water quality, lower greenhouse gas emissions, and reduced energy use.

CE-4 Policies

- CE-4.1 Public Trees.** Require the planting of new trees in conjunction with City-initiated projects, where feasible, and manage and care for all publicly-owned trees.
- CE-4.2 Heritage Trees.** Support the conservation of heritage trees, or trees that are recognized as unique due to their age, rarity, and large size as well as their aesthetic, botanical, ecological, and historic value.
- CE-4.3 Shade Trees.** Promote the planting of shade trees with substantial canopies, and require, where feasible, site design that uses trees to shade buildings, parking facilities, sidewalks, and other facilities to minimize heat island effects and decrease demand for air conditioning.
- CE-4.4 Fruit Tree Program.** Explore the feasibility of implementing a fruit tree program that permits fruit trees to be planted within parks or on another City property. Work with non-profit organizations or community volunteers to identify funding sources (such as grants), plant and maintain the trees, and harvest the fruit for to give to the homeless or low-income families, sell at farm stands or farmers' markets, and/or offer a subscription service to residents that allows the fruit to be delivered to their home or office for a set price.
- CE-4.5 New Development Requirements.** Ensure that new development incorporates and maintains street trees and parking lot plantings as required, and work with residents and businesses to retain healthy trees as part of Indio's streetscape.

Mineral Resources

Goal CE-5: Mineral Resources. Viable long-term production of mineral resource sites that do not adversely impact public health or the environment.

CE-5 Policies

- CE-5.1 Mining Operations Buffers.** Maintain buffers between active mining operations and incompatible land uses, such as housing and commercial development.
- CE-5.2 Reclaimed Mining Sites.** In coordination with the State, periodically declassify and remove mining sites from the State list once a site has been reclaimed.

- CE-5.3 Best Practices.** Coordinate with Riverside County to assure that mining operations continue to implement best available management practices to minimize or avoid nuisances, hazards, or adverse environmental impacts including, but not limited to dust, noise, and erosion.
- CE-5.4 Raw Mineral Resources.** Support efforts to conserve and recycle raw mineral resources.
- CE-5.5 Annexing Active Mining Operations.** Consider the feasibility, benefits, and costs of annexing active mining operations and mineral resource areas with potential for active mining. Conduct a related fiscal analysis that demonstrates that annexation will be revenue neutral or revenue enhancing. Annexation shall not create a revenue drain on the City.

Soils

Protecting and enhancing soil quality through conservation efforts and utilizing best management practices is critically important for sustaining our environment.

Goal CE-6: Soils. The protection of soils from erosion by wind and water, and from the build-up of salts on agricultural lands.

CE-6 Policies

- CE-6.1 Grading.** Minimize grading of land to project specific efforts so as to limit the impact of soil erosion from wind, water, and landslides in areas of unstable slopes, and reduce negative aesthetic impacts in areas of significant landforms.
- CE-6.2 Agricultural Soil Erosion.** Continue to work with agricultural property owners and operators to minimize the impacts of tilling and grading on soil erosion.
- CE-6.3 Agricultural Best Practices.** Promote best agricultural practices regarding to address surface and groundwater contamination, particulate emissions from agricultural operations, minimal soil erosion, and the buildup of salts in soils.

Biological Resources

Maintaining biological diversity is an important component of ensuring the proper function of ecosystems. The conservation of biological resources is integral to ensuring biodiversity and promoting the long-term health of the community and the environment.

Goal CE-7: Biological Resources. The protection and conservation of sensitive biological resources.

CE-7 Policies

- CE-7.1 CVMSHCP and Other Regulations.** Implement the CVMSHCP. Ensure development is consistent with federal, State, and regional regulations for habitat and species protection.
- CE-7.2 Desert Fan Palm.** Work with Riverside County, public and private landowners to maintain and enhance the desert fan palm oases located in the southern slopes of the Indio Hills.

- CE-7.3 Riparian Resources.** Preserve and enhance wetland functions and values to the extent feasible and maintain ecological integrity in order to support riparian resources.
- CE-7.4 Volunteerism.** Encourage community volunteerism and stewardship to help protect and rehabilitate natural resources.
- CE-7.5 Public and Private Partnerships.** Encourage public and private partnerships to acquire and protect habitat areas containing sensitive resources for preservation as permanent open space.
- CE-7.6 Native Plants.** Incorporate native desert plant materials into new development projects to the extent possible and feasible.
- CE-7.7 Sensitive Biological Area Development.** Apply appropriate land use and development regulations to limit development of sensitive biological areas, including wetlands, wildlife movement corridors, and sensitive habitats.
- CE-7.8 Preserve Night Sky.** Ensure that outdoor lighting is shielded and directed away from natural open space areas.
- CE-7.9 Hazards and Open Space.** Maintain open space areas that are designed to protect people and property from risks associated with hazards, such as fault lines, flood zones, high voltage power line areas, and electrical substations.
- CE-7.10 Agency Coordination.** Communicate with neighboring jurisdictions (including the cities of La Quinta, Indian Wells, Palm Desert, Coachella, and the County of Riverside), regional agencies (including the Southern California Association of Governments (SCAG), Coachella Valley Association of Governments (CVAG), and the Local Agency Formation Commission (LAFCO), and Riverside County Transportation Commission (RCTC)), Caltrans, and the SunLine Transit Agency to seek opportunities to improve and expand upon the regional open space/biological preserve system.
- CE-7.11 Aquifer Recharge Areas.** Continue to identify and protect aquifer recharge areas and natural drainages throughout Indio.

Cultural and Tribal Cultural Resources

Identifying and preserving significant cultural and historic resources strengthens community heritage and identity. These resources provide a constant reminder of the culture and history of Indio and the Coachella Valley, and serve as a valuable educational resource for residents and visitors.

Goal CE-8: Historic, Archaeological, and Paleontological Resources. Historic, archaeological, and paleontological resources preserved for their scientific, educational, aesthetic, and cultural values.

CE-8 Policies

- CE-8.1 Site Plan Review.** Ensure adequate site plan review and mitigation measures are implemented for the development of sites with the potential to contain historic, archaeological, and paleontological resources.

- CE-8.2 Avoidance of Impacts to Historic Resources.** For projects that could affect historic resources, ensure adequate study to identify eligible resources and project-level review to avoid or lessen negative impacts through conformance with the *Secretary of the Interior's Standards for the Treatment of Historic Properties*.
- CE-8.3 Incentivize Retention of Historic Landmarks.** Explore opportunities to provide economic and regulatory incentives for the retention and sensitive upgrades and changes to historic landmarks and contributors to designated historic districts.
- CE-8.4 Monitoring.** Require monitoring on sites where grading has the potential to impact subsurface cultural and paleontological resources during excavation and construction activities.
- CE-8.5 Public Education.** Support opportunities to promote public awareness of the history and pre-history of the area as the oldest Valley City and the cultural center of the Coachella Valley.
- CE-8.6 Coordination with Local Tribes.** Periodically meet with representatives from local tribes to:
- Obtain input prior to making decisions, taking actions, or implementing programs/projects that may impact cultural resources;
 - Discuss methods to preserve and protect highly sensitive cultural resources; and
 - Ensure that there is agreement regarding the protocol to be followed when cultural resources are discovered.

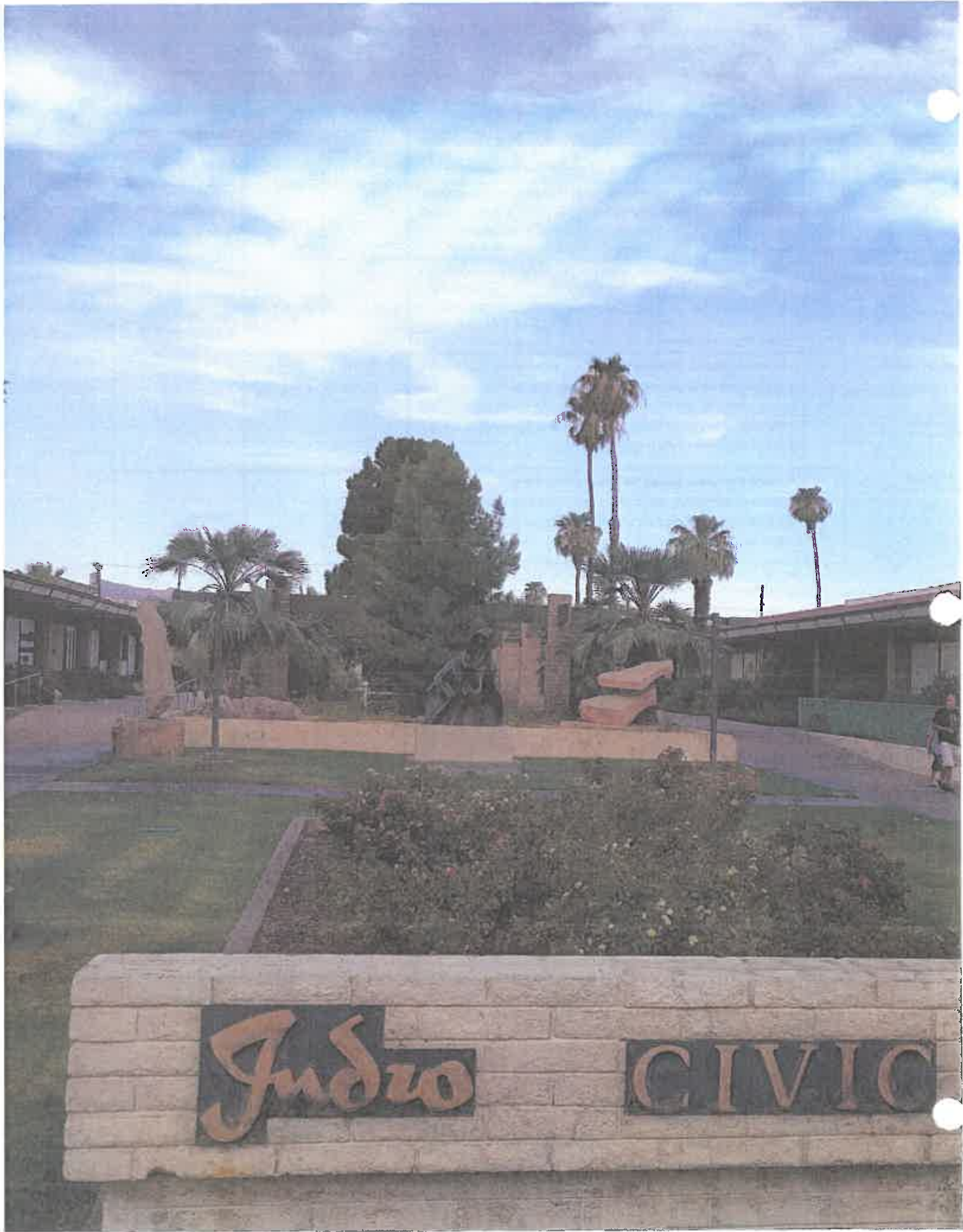
Implementation Actions

The table below identifies programs, policy updates, planning efforts, coordination efforts, and other actions that will help implement the General Plan’s conservation policies. Programs are consistent with this chapter’s goals and policies.

Table 8-4: Conservation Programs

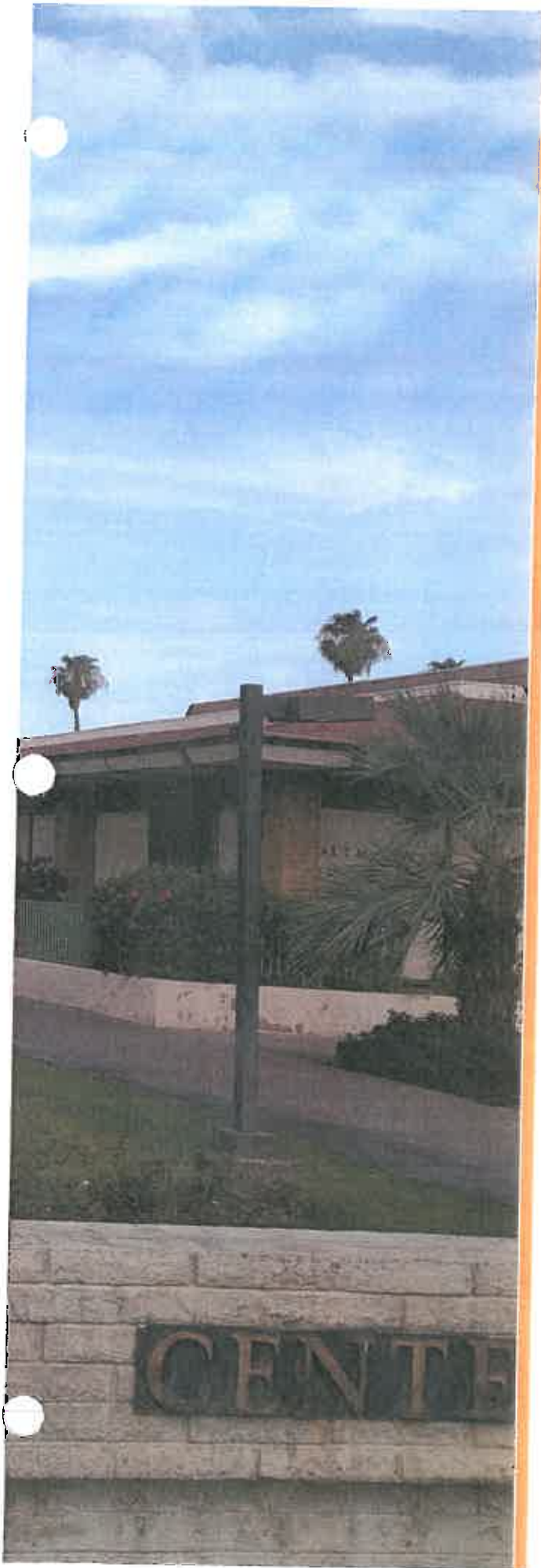
	DESCRIPTION	PRIORITY	TIME FRAME	RESPONSIBILITY
1	<p>Staff Allocation. Designate a City staff member(s) responsible for overseeing implementation of its energy and water conservation programs. Example activities may include:</p> <ul style="list-style-type: none"> • Manage the residential energy disclosure program. • Promote installation of solar panels and implementation of energy upgrades through Indio’s Ygrene and HERO Programs. • Benchmark municipal facilities using the Environmental Protection Agency’s ENERGY STAR Portfolio Manager or equivalent benchmarking tool and identify energy efficiency upgrades in City buildings. • Establish an employee carpooling program. • Provide lectures, seminars, and training on green building based on guide and training material emphasizing desert conditions and opportunities. 	High	Short	Sustainability Coordinator
2	<p>Residential Transfer of Title Energy Disclosures. Establish an energy disclosure program to be performed prior to sale or other transfer of title. Disclosures should provide prospective owners with recommendations for retrofit measures to be given to the buyer prior to transfer of title.</p>	High	Short	Planning, Sustainability Coordinator
3	<p>Low Income Weatherization Assistance Program. Partner with Imperial Irrigation District and SoCal Gas to aggressively promote existing programs that provide financial assistance to low-income households for weatherization improvements and heating, ventilation, and air conditioning (HVAC) tune-ups.</p>	High	Ongoing	Sustainability Coordinator
4	<p>Solar Financing. Formalize and implement a protocol to promote installation of solar panels by continuing to support Indio’s Ygrene and HERO Program and by distributing information on actual savings achieved by PV systems.</p>	High	Short	Sustainability Coordinator
5	<p>Landscape and Water Conservation Ordinance. Adopt the Coachella Valley Model Water Efficient Landscape Ordinance.</p>	High	Short	Planning, Public Works
6	<p>Greywater Ordinance Study and adopt an ordinance allowing greywater systems that redirect water from washbasins, showers, and tubs.</p>	Medium	Medium	Planning, Public Works
7	<p>Zoning and implementation ordinances. Regularly review and update zoning and building codes to enable innovative sustainability measures, including implementation of the following:</p> <ul style="list-style-type: none"> • Permit approval streaming for renewable energy or energy upgrades • Greywater capture and reuse systems • Wind generation on residential and commercial buildings • Electric vehicle infrastructure requirements 	High	Ongoing	Planning, Public Works, Sustainability Coordinator

	<ul style="list-style-type: none"> Green building performance standards, such as those to achieve Net Zero goals Dark Sky Ordinance 			
8	<p>Heritage Trees. Identify heritage trees that should be preserved. Trees on this list shall only be removed when a public interest served by removal outweighs the interest in preservation and heritage status.</p>	Medium	Ongoing	Community Services/Planning
9	<p>Agricultural Study. Develop a study of sound agricultural practices that addresses surface and groundwater contamination, particulate emissions from agricultural operations, minimal soil erosion, and the buildup of salts in soils. Based on the study, create brochures available to the agricultural community.</p>	Low	Short	Community Services
10	<p>Historic Resources Program. Establish a program and identify potential funding sources to facilitate the preservation and maintenance of identified historic resources. Ensure that any proposed changes to such landmarks are appropriate to the historic character of the original structure.</p>	Medium	Short	Planning
11	<p>Historic Resources Design Guidelines. Prepare and adopt design guidelines that contribute to the creation of a distinctive character for each subarea based on important historic/cultural resources within each subarea.</p>	Medium	Short	Planning



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chapter nine

INFRASTRUCTURE AND PUBLIC FACILITIES

The purpose of this Element is to inform and guide future investment in infrastructure and public facilities in the City of Indio. This Infrastructure Element addresses the following support systems important to Indio:

- Water Resources
- Domestic Water Facilities
- Storm drain and flood control facilities
- Wastewater collection and Treatment
- Potable Water Service and Supply
- Electricity and Natural Gas Utilities
- Solid Waste

Key Considerations and Strategies

Infrastructure is the backbone of the community and provides support systems important to the community's development. Storm drains protect properties from flooding. The water system, waste water collection lines, electric power, natural gas, and telecommunications facilities help us live in comfort and are critical to the quality of life in Indio. All these support systems must be sufficient to accommodate the present and future needs the community. When infrastructure ages or growth outpaces capacity, maintenance and infrastructure expansion to meet community needs becomes important. In addition to utility infrastructure, the City operates a number of facilities throughout the City and houses numerous schools and education facilities. Emergency services, including police and fire, are addressed in the Safety Element.

Context

Water Resources and Facilities

The Indio Water Authority (IWA) and the Coachella Valley Water District (CVWD) are responsible for providing water service to Indio. Both water agencies are committed to providing a safe, reliable, and high-quality water supply system that supports current and future generations. IWA's service area covers approximately 38 square miles. It encompasses the majority of Indio and some of the unincorporated sphere of influence. The remaining portion of the planning area is served by CVWD, which has a service area that spans approximately 1,000 square miles from the San Geronio Pass to the Salton Sea. Figure 9-1 identifies these service area boundaries as they relate to Indio's planning area.

Indio's largest water supply source is groundwater from the Whitewater River Basin. This basin has an estimated storage capacity of approximately 30 million acre feet.¹ IWA and CVWD pump groundwater from multiple wells as needed to meet the demands within their service areas. Since the 1930s, groundwater levels have been declining as a result of overdraft, a condition where more water is extracted from the basin than is naturally recharged to it every year. However, in eastern Coachella Valley, groundwater levels have stabilized in recent years. Natural recharge to the groundwater basin is estimated to be approximately 50,000 acre feet per year (AFY), which is only a fraction of annual pumping.²

In addition to groundwater, IWA and CVWD have access to or are working to obtain other water supply sources to meet projected water needs and help eliminate groundwater overdraft. IWA is collaborating with the Valley Sanitation District (VSD) on a recycled water plant that could potentially serve a variety of irrigation customers, including golf courses, parks, schools, and homeowners associations. IWA is also planning a new surface water plant that would treat Colorado River water from the Coachella Canal for potable use and groundwater recharge.³ CVWD relies on a combination of Colorado River water, State Water Project water, surface water, and recycled water. CVWD is also working on a demonstration desalination facility to treat drain water and groundwater for potable and non-potable uses. Additionally, stormwater capture has been identified as a potential method for increasing local water available for

¹ Coachella Valley Water District (2012). Coachella Valley Water Management Plan 2010 Update.

² Coachella Valley Regional Water Management Group (2010). Coachella Valley Integrated Regional Water Management Plan.

³ Indio Water Authority (2012). Indio Water Authority – 2012 Water Master Plan Update.

either groundwater recharge or direct use. Feasible stormwater capture projects would be developed in conjunction with new flood control facilities as development occurs in eastern Coachella Valley.⁴

Water suppliers are required to prepare an Urban Water Management Plan (UWMP) that ensures adequate water supplies are available to meet existing and future water demands. These plans must be updated every five years to support long-term resource planning. They include water demand projections and identify how those demands will be met. While IWA and CVWD are responsible for managing water supplies, the City of Indio has developed goals, policies, and programs that aim to facilitate the sustainable use of water resources.

To continue to meet the growing needs of the City of Indio, IWA and CVWD have partnered with the Coachella Water Authority, Desert Water Authority, and Mission Springs Water District to develop an Integrated Regional Water Management Plan (IRWMP). The IRWMP addresses the Coachella Valley's current and future water needs by paying specific attention to overdraft issues, water supply management, environmental and economic impacts, compliance with State and federal guidelines, and long-term sustainability.

The existing IWA water distribution system consists of 20 groundwater wells, seven storage reservoirs, one large main pressure zone, and two smaller development-based higher zones, which are pumped at the booster pumps at the Shadow Lakes and Terra Lago reservoir. These zones are served by 326 miles of distribution system pipes, which range in diameter from two to 24 inches.

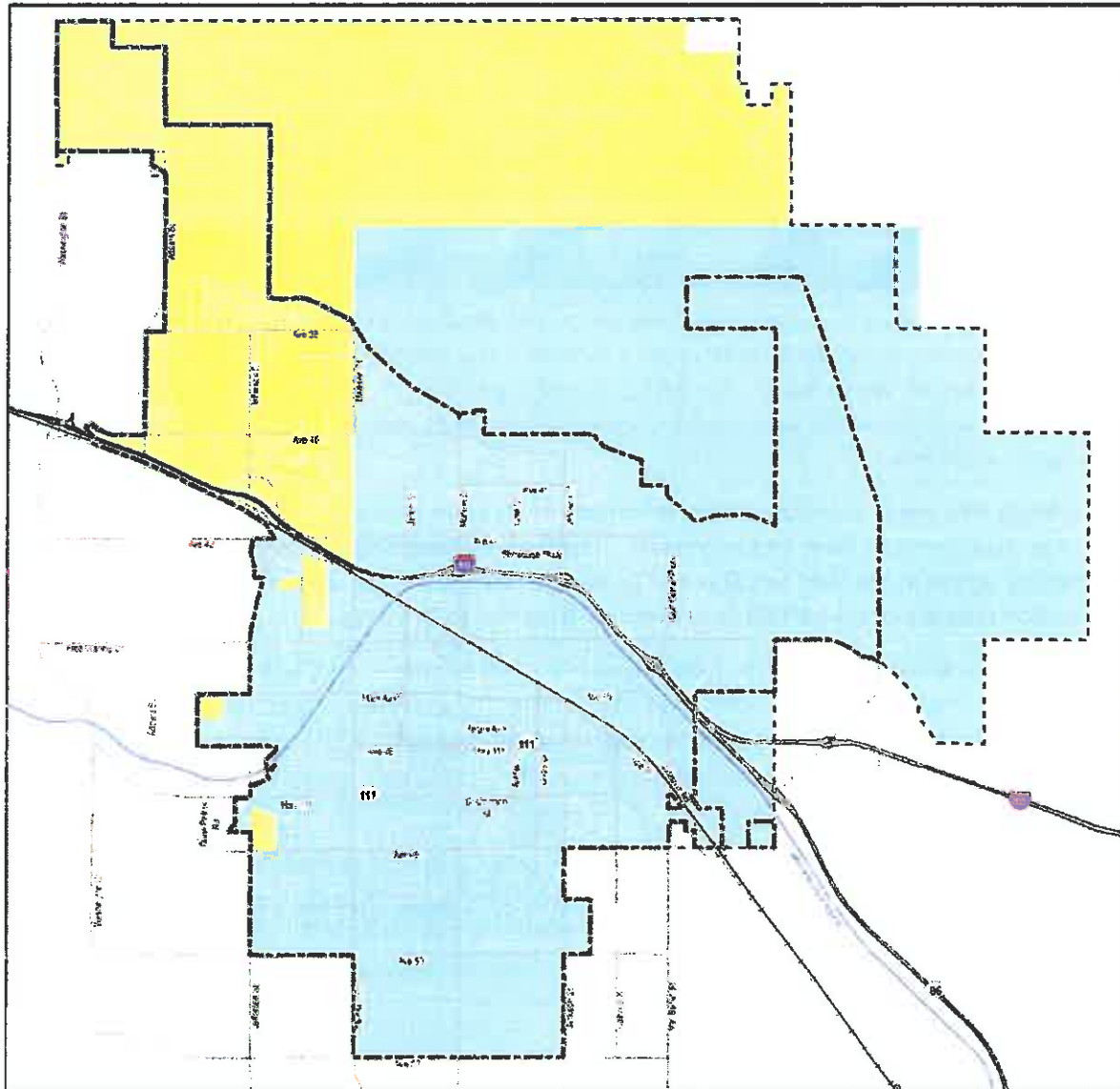
CVWD's domestic water distribution system is nearly 1,990 miles in length, and provides approximately 132,000 AFY to over 280,000 residents. The system consists of approximately 106,000 active meters, 115 deep wells, 59 reservoirs, and 30 pressure zones, which are served by 2,000 miles of distribution system pipes.⁵

IWA and CVWD's existing distribution system is sufficient to meet Indio's current and future demands. Additional pumping, booster systems, and pressure zones will continue to be implemented through Capital Improvement Programs (CIPs) as needed to satisfy the system's performance criteria.

⁴ Coachella Valley Water District (2012). Coachella Valley Water Management Plan 2010 Update.

⁵ Coachella Valley Regional Water Management Group (2012). Coachella Valley Integrated Regional Water Management Plan.

Figure 9-1: Water Service Providers



Water Service Providers



- City Boundary
- Sphere of Influence
- Roads
- Railroads
- Highways
- CVWD Service Area
- IWA Service Area

This map was prepared by the City of Indio, California, and is not intended to be used for any other purpose. The City of Indio is not responsible for any errors or omissions on this map.

This map is not intended to be used for any other purpose. The City of Indio is not responsible for any errors or omissions on this map.

Storm Drain and Flood Control Facilities

Regional drainage facilities are constructed, operated, and maintained by the CVWD. Local drainage facilities are maintained by the City and generally convey runoff from local streets to the regional facilities. The Whitewater River Storm Channel, which is part of the Colorado River Basin Region, is the main regional drainage facility for Indio, eventually draining into the Salton Sea. Flood protection efforts by the CVWD have realigned, widened, and deepened the channel to resolve regional flood problems.

The local storm drain system consists of gutters, engineered storm drains, and channels. The majority of these existing facilities are located south of the Whitewater River Storm Channel and I-10. In 2005, the City prepared a Drainage Master Plan to evaluate the condition of these existing facilities. At that time, a number of areas were identified where flooding or standing water resulted after a storm. Storm drain upgrades were determined for the facilities that could not adequately convey the 10-year storm peak discharge and new storm drains were identified to eliminate observed flooding areas. As with other infrastructure improvements, these recommendations are prioritized and budget is allocated to the projects through the City's CIP to facilitate implementation.

A significant issue associated with drainage is stormwater pollution, which results from daily activities. The most common pollutants are trash, such as fast food wrappers and cigarette butts, and toxins, such as used motor oil, fertilizer, and pesticides. These pollutants enter the Whitewater River Storm Channel untreated as water from rain, hoses, or sprinklers drain from streets, parking lots, and lawns. This pollution can pose serious public health risks due to the pesticides, bacteria, and chemicals that are washed from City streets into nearby water bodies.

To protect against stormwater pollution, Indio requires new development to comply with the requirements of the National Pollution Discharge Elimination System (NPDES). This requires that specific measures will be taken by developers to prevent stormwater pollution before, during, and after construction.



Whitewater River

Wastewater Collection and Treatment

Wastewater service is provided to the City of Indio by the Valley Sanitation District (VSD), which serves 98% of the City's population, and the Coachella Valley Water District (CVWD), which treats the remaining percentage of Indio's wastewater. There are no wastewater services within Indio's sphere of influence.

The existing VSD wastewater collection system consists of approximately 246 miles of sanitary sewer line, five active pump stations, eight siphons, and a wastewater treatment plant. The collection system pipes range in size from four to 54 inches in diameter. Roughly 75% of the gravity sewer system is made up of eight inch or smaller diameter pipes. The oldest known sewer pipes that are still in operation were connected to the system in 1935. Roughly half of the pipes have been constructed within the last 20 years.

VSD operates a wastewater treatment plant at the northeast quadrant of Van Buren Street and Enterprise Way, just southwest of I-10. This plant treats approximately 6.5 million gallons per day (MGD) of wastewater. CVWD also owns and maintains a wastewater treatment facility located at Avenue 38 and Madison Street. This is a tertiary treatment facility and the effluent produced is recycled for nonpotable uses for CVWD customers.

Energy Services

Electrical power within the City of Indio is provided by the Imperial Irrigation District (IID). IID maintains and operates the remission and distribution infrastructure necessary to provide electricity to users throughout the City. The Southern California Gas Company (SCG) is responsible for providing natural gas services to Indio. Both provide reliable and efficient energy services.

Solid Waste and Recycling

Solid waste services are provided by Burrtec Waste and Recycling Services. Burrtec supports a variety of sustainability and zero waste initiatives intended to achieve the State legislature's goal of recycling, composting, or reducing solid waste by 75% by 2020. Converting green waste and food waste to compost, implementing food waste programs, offering educational community workshops on composting, recycling, and relevant City ordinances, and providing participating food establishments with biodegradable bags are some of Burrtec's current practices.



E-waste event in Indio

Public Facilities

The City of Indio operates a number of facilities throughout the City, including City Hall, City offices, Police Station, Fire Station, Indio Community Center, and Indio Teen Center. City Hall is located at 100 Civic Center Mall and is home to many City offices, while City Council Chambers are located at 150 Civic Center Mall. The County of Riverside also manages the Fairgrounds, and Library System Indio Branch.



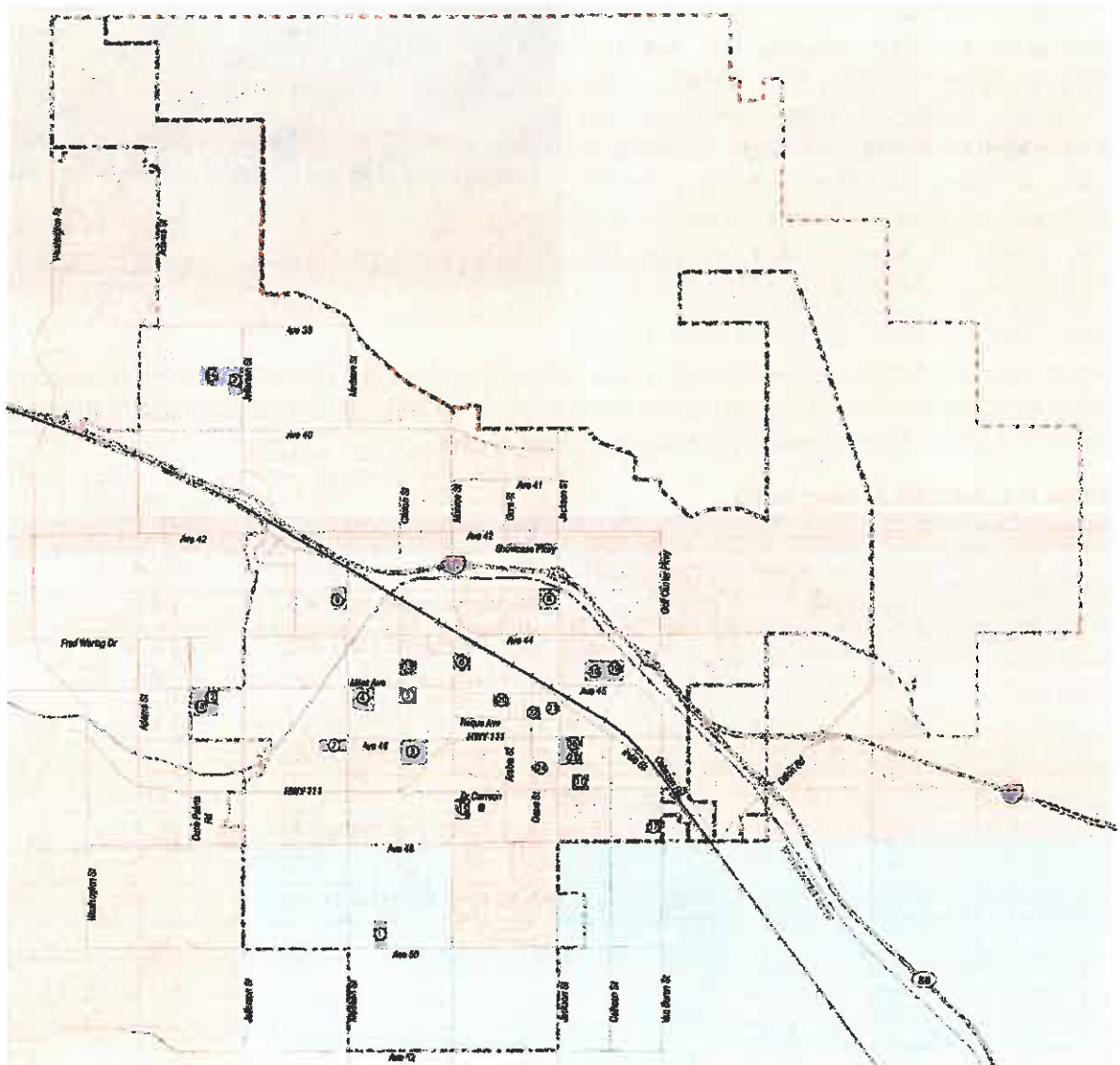
Indio Teen Center

Indio is located within the Desert Sands Unified School District (DSUSD) and the Coachella Valley Unified School District (CVUSD). There are 20 schools in Indio, as shown in Table 9-1. The City is also home to other educational institutions, including College of the Desert, and California Desert Trial Academy College of Law.

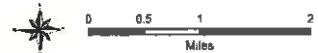
Table 9-1: Schools Serving Indio

	Schools	Address	Grades
1	Mountain Vista Elementary School	49750 Hjorth St, Indio, CA 92201	K-6
2	Shadow Hills High School	39225 Jefferson St, Indio, CA 92203	9-12
3	Indio High School	81750 Ave 46, Indio, CA 92201	9-12
4	Indio Middle School	81-195 Miles Ave, Indio, CA 92201	6-8
5	John F. Kennedy Elementary School	45100 Clinton St, Indio, CA 92201	K-5
6	Herbert Hoover Elementary School	44300 Monroe St, Indio, CA 92201	K-5
7	James Madison Elementary School	80845 Ave 46, Indio, CA 92201	K-5
8	Andrew Jackson Elementary School	82850 Kenner Ave, Indio, CA 92201	K-5
9	Carrillo Ranch Elementary School	43775 Madison St, Indio, CA 92201	K-5
10	Dwight Eisenhower Elementary School	83391 Dillon Ave, Indio, CA 92201	K-5
11	Theodore Roosevelt Elementary School	83200 Dr Carreon Blvd, Indio, CA 92201	K-5
12	John Glenn Middle School of Intl Studies	79655 Miles Ave, Indio, CA 92201	6-8
13	Lyndon B Johnson Elementary School	44640 Clinton St, Indio, CA 92201	K-5
14	Desert Ridge Academy Middle School	79767 Ave 39, Indio, CA 92203	6-8
15	Dr Reynaldo J Carreon Jr Academy	47368 Monroe St, Indio, CA 92201	K-5
16	Thomas Jefferson Middle School	83-089 CA-111, Indio, CA 92201	6-8
17	Martin Van Buren Elementary School	47733 Van Buren St, Indio, CA 92201	K-5
18	Amelia Earhart Elementary School	45250 Dune Palms Rd, Indio, CA 92201	K-5
19	Amistad Continuation High School	83501 Dillon Ave, Indio, CA 92201	9-12
20	Our Lady of Perpetual Help School	82470 Bliss Ave, Indio, CA 92201	Preschool, K-8
21	California Desert Trial Academy College of Law	45-290 Fargo St, Indio, CA	College of Law
22	College of the Desert	45524 Oasis St, Indio, CA 92201	Community College
23	Desert Sand Adult School	83089 CA-111, Indio, CA 92201	Adult School
24	Don F. Kenny Regional Learning Center	47-336 Oasis St, Indio, CA 92201	Middle and High School, Independent Studies, Adult Transition Program

Figure 9-2: Schools and School District Boundaries



Schools and School District Boundaries



- City Boundary
- Sphere of Influence
- School District
- Coachella Valley District
- Schools
- Schools
- Roads**
- Railroads
- Highways

Source: Riverside County AECOM (City Boundary, 2012, 600,612)
 Riverside County TMA (Roads/Railroads/Highways); Riverside County GIS

The information on this map was derived from various digital databases, located above. Care was taken in the creation of the map but it is intended "as is". PDC Limited, 2000, are responsible for any errors, omissions, or published accuracy and therefore, there are no warranties which accompany this product. Users are advised to read carefully and not rely on this product before making any decisions.

Goals and Policies

Surface and Subsurface Water Resources

The City will implement best practices and not only replenish, but improve the quality of surface and subsurface water resources in order to support water needs of present and future generations.

Goal IE-1: Surface and Subsurface Water. Surface and subsurface water resources that meet or exceed established water quality standards.

IE-1 Policies

- IE-1.1 **Water Supply.** Work with CVWD and IWA to ensure an adequate supply of domestic water facilities to sustain existing and projected water needs.
- IE-1.2 **Water Facilities.** Ensure adequate funding is available to accommodate existing and future water facilities.
- IE-1.3 **NPDES Compliance.** Ensure project developer compliance with the National Pollution Discharge Elimination System (NPDES) MS4 permit requirements.
- IE-1.4 **Promote Public Awareness.** Continue to work with co-permittees of the NPDES permit to promote public awareness of water quality issues.
- IE-1.5 **Agricultural Contamination.** Work with the agricultural community to reduce or eliminate surface and groundwater contamination due to agricultural activities.
- IE-1.6 **Public Awareness.** Use agency websites, public service announcements, and other means to inform the public about water quality issues and methods to prevent contaminants from entering the storm drain system.

Goal IE-2: Storm Drain and Flood Control. Well-designed storm drain and flood control facilities that benefit the community and protect sensitive streams and watercourses.

IE-2 Policies

- IE-2.1 **Facility capacity.** Ensure that local storm drain and flood control facilities are designed with sufficient capacity to protect the public and property from stormwater damage.
- IE-2.2 **Stormwater Facility Funding.** Ensure adequate funding is available to maintain and improve existing local stormwater facilities
- IE-2.3 **Storm Drain Master Plan.** Periodically review and update the City's Storm Drain Master Plan and Hydraulics Analysis to determine the facilities and improvements necessary to adequately service existing and future demand.
- IE-2.4 **Multiple Use.** Promote multiple use of flood control facilities where feasible, combining for example retention basins and recreational facilities.

- IE-2.5 Agency Coordination.** Work with the CVWD when regional stormwater facility projects are planned in or adjacent to Indio.
- IE-2.6 Stormwater Facility Design.** Design storm drain facilities to allow for infiltration, evapotranspiration, or reuse of stormwater on the site where it is generated to the extent practical.
- IE-2.7 Mitigation Measures for New Development.** Restrict, or where feasible, require adequate mitigation measures for any development of habitable structures within watercourses and/or other stormwater facilities.
- IE-2.8 Flood Control Facilities.** Allow flood control facilities to be developed in conjunction with compatible recreational facilities or other land uses that are not susceptible to flood damage.
- IE-2.9 State and Federal Guideline.** Construct and maintain storm drains and flood control facilities in accordance with local, State, and federal guidelines.

Wastewater Collection and Treatment

The City will maintain a safe, efficient, and adequate wastewater system to meet the needs of current and future populations.

Goal IE-3: Wastewater Collection. Wastewater facilities that effectively collect and treat wastewater.

IE-3 Policies

- IE-3.1 Agency Coordination.** Work with the Valley Sanitary District (VSD), CVWD, and IWA to ensure reasonable and adequate wastewater capacity is available to serve current and future demand and pursue opportunities to utilize reclaimed and recycled water.
- IE-3.2 Wastewater Facility Funding.** Ensure that adequate funding is available to maintain and improve existing facilities as well as construct new infrastructure as needed.
- IE-3.3 Sustainable Wastewater Systems.** Continue to provide innovative and sustainable wastewater systems, such as the biological wetlands at the Coachella Valley Wild Bird Center, that both treat wastewater and serve as habitat for waterfowl and shorebirds.
- IE-3.4 Greywater Systems in New Development.** Encourage new residential development to provide easy implementation of greywater systems that redirect water from washbasins, showers, and tubs for use in toilet flushing, irrigation, and other non-potable uses.
- IE-3.5 Agency Coordination.** Work with the Coachella Valley Water District (CVWD), Valley Sanitation District (VSD), and the Indio Water Authority (IWA) to ensure that reclaimed water is available and used where appropriate and disseminate information about water conservation measures.

Electricity and Natural Gas Utilities

The City supports a more diverse portfolio of energy sources, particularly as renewable sources of energy, such as biomethane, solar, and wind power. The City will also ensure that electricity and natural gas services are available to the community.

Goal IE-4: Energy. Efficient electricity and natural gas utilities that ensure the availability of these resources for future generations.

IE-4 Policies

- IE-4.1 Agency Coordination.** Work with the Imperial Irrigation District (IID) and the Southern California Gas Company (SCG) to ensure that cost-effective utilities are provided, maintained, and operated in a safe manner.
- IE-4.2 Environmental Review.** Require the appropriate environmental review and clearance of electricity and natural gas infrastructure projects including solar generation, cogeneration facilities, wind energy systems, and similar developments to ensure proper siting and operation.
- IE-4.3 Methane Reuse.** Partner with SCG and VSD to find a productive use for the methane produced at the wastewater treatment facility in Indio.
- IE-4.4 Below-Ground Utilities.** Phase out and replace overhead electric lines with subsurface lines that will not be affected by fallen trees and branches during windstorms.
- IE-4.5 Tree Trimming.** Enforce the national guidelines on tree trimming and vegetation management around electric transmission and communication lines to prevent or reduce the potential for felled branches or trees to cause power outages and disrupted communications.

Telecommunications

In an effort to attract business, institutional uses, and industry, Indio will support high quality telecommunication services and utilities.

Goal IE-5 Telecommunications. High-quality telecommunication services and utilities to meet the needs of residents and businesses.

IE-5 Policies

- IE-5.1 Public-Private Broadband Partnership.** Actively seek a public-private partnership to provide ultra-high speed fiber optic communications to businesses in Indio. When feasible and applicable, new construction shall install future cables to support citywide broadband internet accessibility.
- IE-5.2 Telecommunication Facility Siting.** Ensure that siting of telecommunication facilities provides efficiency and quality services to emergency response providers in the City.
- IE-5.3 Utility Undergrounding.** Work with local utility providers and provide assistance in facilitation of complete community undergrounding of all utility distribution lines.
- IE-5.4 Visual Impacts.** Power and other transmission towers, cellular communication towers, and other viewshed intrusions shall be designed and sited to minimize environmental hazards and visual impacts.

Solid Waste and Recycling

Indio strongly supports efforts to reduce, reuse, and recycle to divert as much solid waste as possible from landfills. The City is dedicated to maintaining a clean and safe environment.

Goal IE-6: Solid Waste and Recycling. Maximum diversion of solid waste from landfills through the reduction, reuse, and recycling of materials.

IE-6 Policies

- IE-6.1 Solid Waste Generated from Landfills.** Develop strategies to divert a minimum of 75% of the solid waste generated from landfills through source reduction, recycling, and composting, with an ultimate objective of achieving zero waste whereby all discarded materials are designed to become resources for other uses.
- IE-6.2 Waste Collection.** Work with Burrtec to expand residential and commercial services to include food waste collection in addition to recycling, green waste, and solid waste pick-up. Initiate food scrap program at City facilities.
- IE-6.3 Educate the Public.** Provide information to the public on composting, recycling, and “green purchasing,” which refers to buying environmentally-friendly products and services that utilize fewer resources, are designed to last longer, and minimize impacts to the environment from production to disposal.
- IE-6.4 Commercial Waste Audit.** Work with Burrtec to continue to provide free waste audits to commercial customers
- IE-6.5 Waste Management Facilities.** Acknowledge that material recovery facilities and transfer stations are an important and fundamental component of effective waste management and allow such facilities to locate within limited and appropriate locations in employment districts where they will not negatively impact adjacent land uses.
- IE-6.6 Community Reuse.** Support community-based programs that promote food sharing, electronics recycling, and the reuse of consumer goods.
- IE-6.7 Zero Waste Government Operations.** Strive for zero waste government operations, modeling best practices in solid waste management and recycling for the rest of the community.
- IE-6.8 Recycled Building Material.** Encourage the use of recycled building and infrastructure materials in new public and private development.
- IE-6.9 Paper Waste Reduction.** Reduce paper waste and encourage the use of recycled paper in City operations.
- IE-6.10 Neighborhood Cleanup.** Encourage neighborhood pride and cleanup events.
- IE-6.11 Street Sweeping.** Continue to provide regular street sweeping for residential, commercial, and industrial areas.

Public Facilities

The City will maintain high-quality public facilities and schools for Indio residents and visitors.

Goal IE-7 Educational Opportunities. High-quality educational opportunities for lifelong learning.

IE-7 Policies

- IE-7.1 Future Demand.** Coordinate with the local school districts in identifying potential school sites needed to meet future demand, as well as the planning, site acquisition, and development of educational facilities in the City.
- IE-7.2 Higher Education.** Prioritize well-planned, higher-educational facilities in Indio, including satellite university campuses and vocational training schools in medical research, technology, and other trade areas as needed to support local industry needs.
- IE-7.3 Library Space.** Continue to work with Riverside County to ensure adequate library space, services, books, and other resources are available to residents and students.
- IE-7.4 Quality Early Education.** Work with local school districts, private schools, pre-schools, and other agencies to maximize educational quality.
- IE-7.5 Senior and Teen Centers.** Continue to support Indio's Senior and Teen Centers as a local resource for youth and seniors.
- IE-7.6 Safe Youth Programs.** Prioritize programs for youth that serve as safe after school activities.

Implementation Actions

The table below identifies programs, policy updates, planning efforts, coordination efforts, and other actions that will help implement the General Plan’s infrastructure and public facilities policies. Programs are consistent with this chapter’s goals and policies.

Table 9-2: Infrastructure and Public Facilities Programs

	DESCRIPTION	PRIORITY	TIME FRAME	RESPONSIBILITY
1	Plan Consistency. Following adoption of the General Plan, review the existing utility master plans and make changes to ensure consistency with the General Plan.	High	Short	Public Works
2	City Ordinances. Update the existing City code and ordinances to reflect current requirements and GP policies for water, sewer, and storm drainage requirements and policies. This should include low-impact development practices which improve water quality and promote groundwater recharge, increase on-site infiltration.	High	Short	Public Works, Planning
3	Groundwater Management Plan. Complete the Groundwater Management Plan, as required by State law. Oversee the completion of the Plan and provide materials and resources as needed.	Medium	Short	Public Works
4	Solid Waste Diversion. Establish a waste diversion plan with in a minimum diversion rate of 75% of solid waste generated from landfills.	High	Short	Public Works
5	Multifamily Recycling. Establish a protocol to expand outreach and technical assistance to multifamily buildings to increase recycling in accordance with AB341.	Medium	Short	Public Works
6	Green Purchasing. Implement a “green purchasing” program that minimize impacts to the environment from production to disposal. The program should address product stewardship and producer responsibility.	Medium	Short	City Manager
7	Food share programs. In conjunction with community partners and neighborhood organizations, explore and pilot food share programs reduce food waste.	Low	Medium	Planning, Public Works.

8	Event waste diversion. Require recycling and food waste diversion at special events, including City-hosted events.	High	Short	Public Works
9	Neighborhood Clean-up Program. In coordination with the Housing and Development Services Department, host community and neighborhood clean-up program, tree planting, and other beautification programs.	Low	Short	Planning, Public Works.
10	Development impact fees. Adopt nexus-based impact fees that mitigate the cost of providing infrastructure and public facilities to serve new development. Address infrastructure improvements that are necessitated by new development – including water, sewer, stormwater, and utilities. Seek additional funding mechanisms as necessary.	High	Ongoing	Planning, Public Works
11	Sewer and Stormwater user fees. Update existing user fees for sewer and stormwater to fund needed system upgrades and, to the extent feasible, allow for waste water recycling and stormwater capture for beneficial use.	Medium	Medium	Planning

NO PARKING
EXCEPT FOR
EMERGENCY
VEHICLES

INDIO

POLICE

Our Community.
Our Commitment.





chapter ten

SAFETY

The purpose of the Safety Element is to establish a policy framework for maintaining and improving the safety of Indio's residents. This Element seeks to strengthen links between quality community design and safety issues. It also identifies known seismic, flooding, and geological hazards, as well as methods to reduce the potential risk of illness, injury, death, or property damage that can occur as a result of these hazards. Crime prevention, police and fire protection, and emergency preparedness and response are also addressed within this Element.

Key Considerations and Strategies

Indio is at risk to a number of natural and man-made hazards. The City’s Local Hazard Mitigation Plan (LHMP) explores the severity, probability, and ranking of all hazards in the City, and identifies risks and vulnerabilities from hazards, such earthquakes, floods, agricultural hazards, technological hazards, and extreme weather. Table 10-1 shows the ranking of hazards in Indio and Riverside County as listed in the City’s Local Hazard Mitigation Plan.

Table 10-1: Hazard Ranking in Indio

HAZARDS/THREATS	COUNTY			LOCAL JURISDICTION		
	SEVERITY 0 - 4	PROBABILITY 0 - 4	RANKING 1-23	SEVERITY 0 - 4	PROBABILITY 0 - 4	RANKING 1-20
1. Earthquake	4	2	1	4	4	1/1
2. Wildland Fire	3	4	3	2	1	15/16
3. Flood	3	3	9	3	3	7/8
4. Drought	3	3	11	4	3	5/9
5. Landslide	3	3	21	1	1	17/22
6. Insect Infestation	2	3	18	3	3	8/17
7. Tornado	2	1	17	3	1	-/20
8. Extreme Weather	3	2	13	4	4	2/2
9. Pipeline Disruption	3	2	20	3	3	13/19
10. Aqueduct Event	3	2	16	3	3	14/15
11. Transportation Failure	3	2	14	3	3	9/11
12. Electrical Failure	4	4	4	3	4	4/4
13. Hazardous Materials Incident	3	4	22	3	3	19/6
14. Nuclear/Radiological Incident	4	1	12	4	1	11/21
15. Terrorist Event	3	1	7	4	3	12/6
16. Civil Disorder	3	2	10	1	1	18/12
17. Jail/Prison Event	2	1	19	2	1	19/13
18. Communications Failure	3	2	6	4	2	--/3
19. Cyber-Attack	2	4	6	4	3	--/7
20. Water Supply Disruption/Contamination	2	3	23	3	2	-/10
21. Emergent Disease/Contamination	3	3	5	2	1	3/14
22. Pandemic Flu	4	2	2	3	2	4/18
23. Dam Failure	3	1	15	-	-	-/-

2012 City Ranking /2017 City Ranking

Source: The information reflects estimations made in 2017 (City of Indio 2017 Local Hazard Mitigation Plan).

Climate change is anticipated to be a risk amplifier for many of these natural hazards. For example, warmer temperatures and changes in precipitation patterns may increase the length, intensity, and frequency of drought. Similarly, the City is projected to experience more frequent, more intense, and

longer heat waves.¹ These extreme weather events will place more people, particularly seniors, children, and outdoor workers at increased risk of illness and death. Pursuant to Senate Bill 379 (SB 379), updates to the Safety Element must also address climate adaptation and resiliency strategies.

Context

Police Services and Crime Prevention

The Indio Police Department is located at 46800 Jackson Street and currently has a staff of approximately 80 employees. The Department is composed of the Field Services Division and the Support Services Division. These two Divisions fall under the Office of the Chief, which is charged with communicating the overall direction, mission, and goals to all areas of the Police Department. The Field Services Division is responsible for controlling crime and public safety issues throughout Indio, investigating traffic collisions, enforcing traffic violations, participating in community outreach efforts, operating the K-9 Teams, Code Enforcement, and the School Resource Officer (SOR) program. The support Services Division is responsible for supporting the Department's policing activities and initiatives. The Division consists of Police Investigations, the Communications Unit, Information Technology (IT), and Property and Evidence Unit.

The Indio Police Department uses a variety of strategies to address crime. The Department has a long-standing and successful tradition of maintaining positive relationships with members of the community through effective community outreach and public engagement strategies. The Department embraces a community-oriented policing philosophy that supports the systematic use of partnerships and innovative techniques to proactively address the conditions that give rise to public safety issues.

Fire and Emergency Medical Services

Indio's Fire Department is committed to protecting all lives and property within the City from the adverse effects of fire, medical emergencies, and hazardous conditions. A full range of cost-effective fire services are provided in the City as part of the regional fire protection system through a cooperative agreement with the Riverside County Fire Department. The City provides a full-service Fire Department currently equipped with approximately 56 full-



Indio fire station

time staff. Fire services are delivered from four stations strategically located throughout the City to provide timely response and services.

¹ This information was gathered from the internet based Cal-Adapt planning tool, which was developed to assist local jurisdictions in identifying the risks that climate change pose to specific geographic areas. Go to <http://cal-adapt.org> for additional information.

The Indio Fire Department offers a Fire Explorer program, a volunteer program, and a Prevention Office.

Emergency Preparedness

The City of Indio's public safety personnel plan and train for all types of emergency and disaster situations that could affect the health and safety of Indio's residents, visitors, business owners, and pass-through commuters. The Community Emergency Response Team (CERT) Program, managed through the County of Riverside Emergency Management Department, educates the public about disaster preparedness. CERT also trains citizens in basic emergency skills, including team management, fire safety, light search and rescue, and basic medical practices. California Government Code Section 3100-3101 states that all public employees are declared to be disaster service workers in the event of a natural or human-made disaster.

The City of Indio's Emergency Operations Center is located at 45222 Towne Street and there is an alternate Center located at 83101 Avenue 45. Emergency Services volunteers provide support to the City's Emergency Operations Center, support community presentations and public safety events, serve as emergency amateur radio operators, and support emergency management field activities. Indio is also home to the Riverside County East County Emergency Operations Center, which is located at 82695 Dr. Carreon Boulevard. This high-tech facility is where regional operations are coordinated in the event of a natural, technological, intentional human-made and unintentional human-made disaster.

Hazardous Materials/Brownfields

As cities age, land uses associated with hazardous materials are often abandoned. These uses include former industrial properties, gasoline stations, and military sites. Uses such as this may have soils and groundwater that are contaminated and are often referred to as "brownfields."

Indio has few brownfield or hazardous material sites. One active voluntary cleanup site is located at a metal manufacturer. Indio High School has completed a cleanup and is now certified. Five schools within the City have been investigated and no additional remediation is necessary, and two other sites are waiting for investigation.

Hazardous materials facilities are regulated by the Environmental Protection Act of 1970 and various others with oversight from the Riverside County Department of Environmental Health Hazardous Materials Branch.

Geologic and Seismic Hazards

There are three major, known faults located in Riverside County: the San Andreas, San Jacinto, and Elsinore faults. The San Andreas and San Jacinto are two of California's most active faults. Several properties within Indio and its sphere of influence are directly impacted by the southernmost section of the San Andreas fault and/or secondary faults and fractures, as shown on Figure 10-1. These faults pose geologic and seismic hazards in the form of earthquakes, fault rupture, liquefaction, and landslides. The most recent earthquake along this stretch of the fault occurred more than 300 years ago leading scientists

to suggest that it has accumulated a substantial amount of tectonic stress and is likely to produce a large (7 to 8 magnitude) earthquake in the near future.²

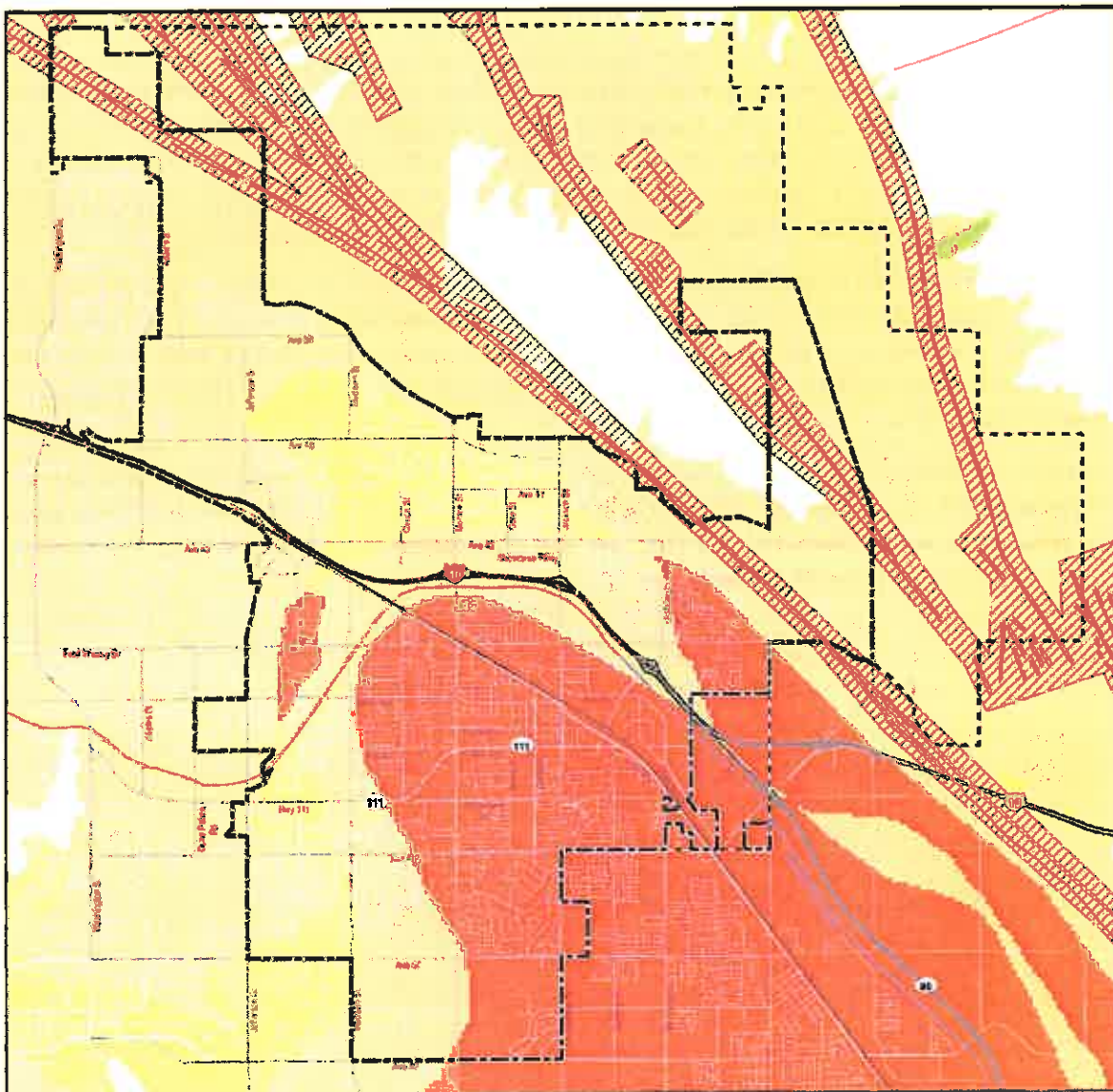
An earthquake involves the rapid shaking of the ground, which alters the position of the earth's tectonic plates. Earthquakes can also result in fault rupture, which occurs when movement on a fault deep within the earth breaks through to the surface creating an offset in the ground as the two sides of the fault slip past each other. The intense shaking of an earthquake can cause damage and lead to the collapse of buildings and structures. A structure's ability to withstand the magnitude of an earthquake is often dependent on when and where it was built, and whether it was built to seismic codes.

Liquefaction, most often caused by earthquakes, describes a phenomenon where a soil's strength and stiffness is substantially reduced. Liquefaction causes the soil's composition to liquefy, which destabilizes buildings that are supported by the ground. Indio could experience seismic shaking levels that have the potential for liquefaction in areas where groundwater is generally shallower than 30 feet. These areas are shown on Figure 10-1.

Landslides occur when masses of rock, earth, or other material move rapidly down a slope. Landslides and surficial slope failure are most likely to occur in areas with a slope greater than 25 percent (hillside areas) and along steep bluffs. Since Indio is relatively flat with undeveloped hillsides along the northern boundary, the City is at low risk for landslides.

² Philibosian, B., Fumal, T., and Weldon, R. (2011). San Andreas Fault Earthquake Chronology and Lake Cahuilla History at Coachella, California. Bulletin of the Seismological Society of America, Vol. 101, No. 1.

Figure 10-1: Seismic Hazards Map



Seismic Hazards



- | | |
|---------------------|------------------------------------|
| City Boundary | Earthquake Faults |
| Sphere of Influence | Faults |
| Roads | Fault Zones |
| Railroads | General Ground Shaking Risk |
| Highways | Very Low |
| | Low |
| | Moderate |
| | High |

Source: MapInfo, 2011; USGS, 2012; Boreas, 2012; NCEM, 2014; The City of Indio, 2014; PA, 2014; Bureau of Geomatics, 2014.

This map shows the results of a seismic hazard analysis. It is not intended to be used as a basis for engineering or other professional services. It is not intended to be used for any purpose other than that for which it was prepared. The City of Indio is not responsible for any errors or omissions in this map or for any consequences arising from its use.

Flood Hazards

The LHMP identified flooding as having above average severity and above average probability. Portions of the City are located within the 100-year floodplain, including the northwest portion of the City, north of the I-10 Freeway, and within the Whitewater River. Some isolated areas within the northwestern and southwestern portion of the City fall within the 500-year floodplain.³ Figure 10-2 illustrates the area within the 100-year and 500-year floodplain.



Flooding in Indio

Flash flooding is another form of hazardous flooding. Flash floods occur when excessive water fills normally dry creeks or river beds along with currently flowing creeks and rivers, and typically combines the destructive power of a flood with speed and unpredictability.⁴ Populations most at risk during flash flooding may include hikers, homeless, and children.

The Coachella Valley Water District (CVWD) recently approved a regional flood project that will provide protection for northeast Indio and the surrounding area. This North Indio Regional Flood Control System (NIFCS) will collect flood flows from the outlets of the Sun City Palm Desert Whitewater River/stormwater channels and convey them to the Sun City Shadow Hills channels, which will ultimately connect to the Coachella Valley Storm Channel. The second phase of this project includes obtaining levee accreditation from FEMA for the existing East Side Dike. This will reduce the likelihood of the hazard.

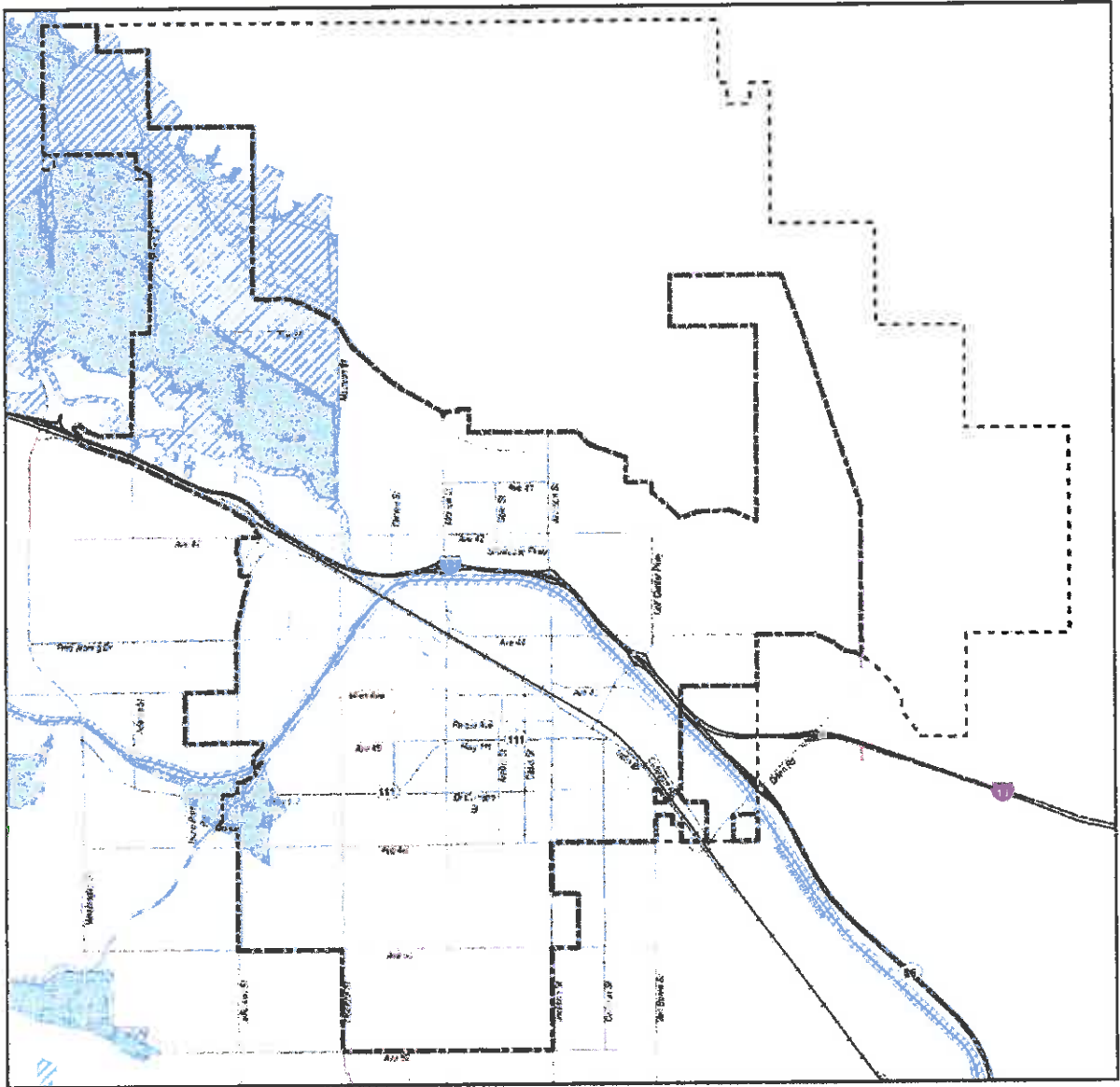
Additionally, the City is currently replacing an existing at grade low-water crossing where Avenue 44 crosses the Coachella Valley Storm Channel with a permanent, elevated all-weather bridge. The bridge will reduce the possibility of overtopping of the roadway during flood events, enhance emergency response, and improve overall road safety.

Indio does not have the risk of dam failure because no true dams exist; however, levees and dikes are types of dams. Indio also has earthen retention basins on the north side of the City, which pose a threat for failure and subsequent flooding. In addition, Riverside County does contain several local dams and reservoirs, dam failure inundation zones are limited to the western region of Riverside County.








³ The Federal Emergency Management Agency (FEMA) defines a 100-year flood zone as having a 1% chance of flooding each year, while areas in a 500-year floodplain have a 0.2% chance of flooding in any given year.

⁴ The National Severe Storms Laboratory (NSSL) Flood Basics: <https://www.nssl.noaa.gov/education/svrwx101/floods/>

Figure 10-2: Flood Hazards



Flood Hazards

-  City Boundary
-  Sphere of Influence
-  Roads
-  Railroads
-  Highways
-  FEMA FIRM 100-Year Floodplain
-  FEMA FIRM 500-Year Floodplain



Source: Department of Public Works, City of Indio, California. The City of Indio is not responsible for the accuracy of the information presented on this map. The City of Indio is not responsible for any errors or omissions in this map. The City of Indio is not responsible for any damages or liabilities arising from the use of this map. The City of Indio is not responsible for any damages or liabilities arising from the use of this map.

Climate Change

Climate change presents Indio with a series of challenges. Climate change impacts pose an immediate and growing threat to the economy, environment, and public health. Cities like Indio will continue to experience the effects of climate change, including the increased likelihood of droughts, worsening air quality, increased flooding, and heatwaves.⁵

These climatic changes may result in significant social, economic, and environmental issues and opportunities for residents and businesses in Indio in the long term, including:

- + **Public health impacts:** Indio will experience longer, more frequent, and more severe heat waves, increasing the risk of heat-related morbidity in vulnerable populations. An increase in regional wildfires will further worsen air quality.
- + **Flood impacts:** Stormwater infrastructure may require costly upgrades and increased maintenance costs in order to accommodate more intense rain storms.
- + **Drought:** Changes to the amount and timing of rainfall and regional groundwater levels may threaten already limited regional water supply. Changes in precipitation can impact the production of staple crops, impacting the quantity and quality that is available, potentially increasing price.

The following section summarizes key information about climate changes in the region.

Temperature and Extreme Heat

During the last century, temperatures in California rose steadily. Between 1918 and 2006, the average minimum temperature increased by 0.3°F per decade, and the average maximum temperature increased by 0.13°F per decade. The rate of warming intensified from 1970 to 2006, with average minimum temperatures increasing 0.56°F per decade and average maximum temperatures rising 0.49°F per decade. Average minimum and maximum temperatures in Southern California rose faster than the State as a whole. Between 1970 and 2006, the average minimum temperature rose by 0.67°F per decade and the average maximum temperature increased by 0.74°F per decade across the region.⁶

Models indicate that temperatures will continue to rise in the Indio. Annual maximum temperatures are projected to increase between 4.7°F and 5.8°F by mid-century (2040-2060) and 5.6°F and 9.1°F by end of century (2080-2100).⁷ By the end of the century, average temperatures are anticipated to fall outside of the annual variability range seen in the historic record, particularly in the summer and fall. More simply put, the average future temperature in the climate scenario with the least warming is greater than the

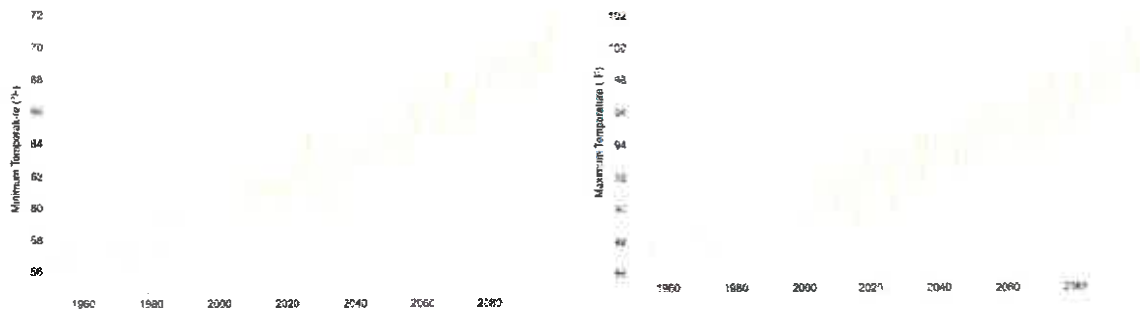
⁵ California Natural Resources Agency (2017). Safeguarding California. <http://resources.ca.gov/wp-content/uploads/2017/05/DRAFT-Safeguarding-California-Plan-2017-Update.pdf>

⁶ Cordero, E. C., W. Kessomkiat, J. Abatzoglou, and S. A. Mauget. (2011). the identification of distinct patterns in California temperature trends. *Climatic change* 108:357– 382.

⁷ California Energy Commission. (2017). Cal-Adapt. Available at <http://cal-adapt.org/>.

very warmest year of the historic record.⁸ Figure 10-3 shows the projected change in average annual minimum and maximum temperatures.

Figure 10-3: Projected Change in Average Annual Minimum and Maximum Temperatures in Indio



Note: Business as Usual Scenario (High Emissions), CanESM2 Model (Average)
Source: CalAdapt

With climate change, extreme heat events in California and Indio are becoming more frequent, more intense, and longer lasting. An extreme heat day is defined as a day between April and October when the maximum temperature exceeds a heat threshold. This threshold is often calculated as the 98th percentile of historical maximum temperatures between April 1 and October 31 based on observed daily temperature data. For Indio, this threshold is 104°F.

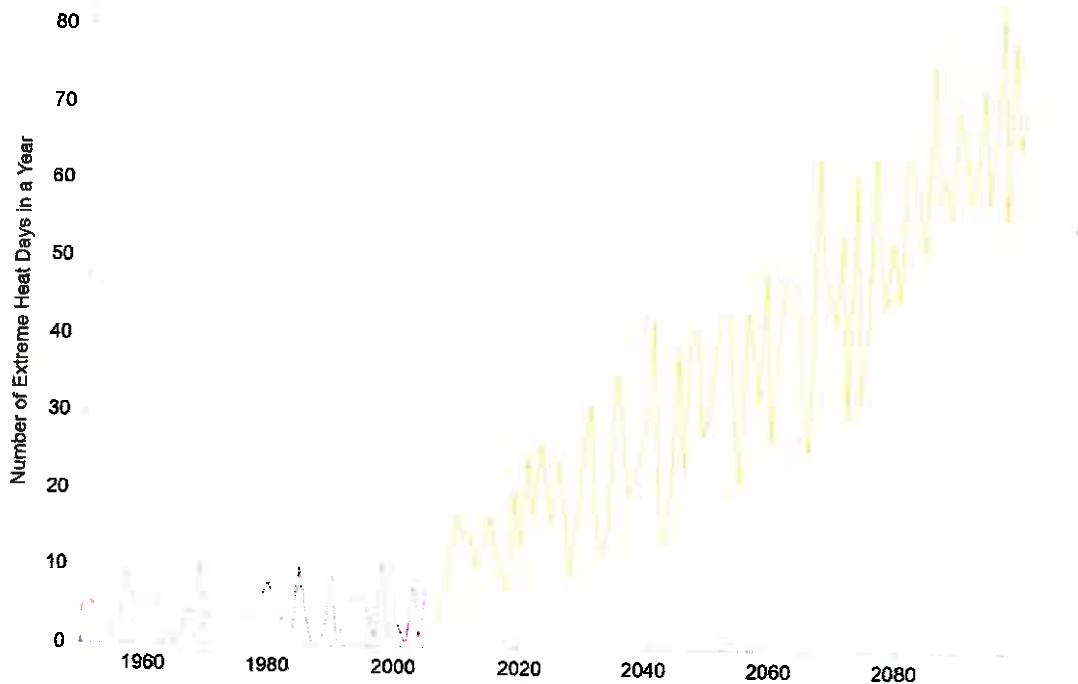
Between 1950 and 2005, Indio experienced, on average, about four extreme heat days per year. The number of extreme heat days is anticipated to increase significantly across the Coachella Valley region during the next century. Under a business-as-usual scenario, by mid-century Indio is expected to have 25 extreme heat days. By end of century, Indio is projected to experience 51 extreme heat days. Figure 10-4 shows the projected number of extreme heat days in Indio. More extreme heat days can pose a serious threat to human health, resulting in an increase in emergency department visits and hospitalizations⁹ and deaths.¹⁰

⁸ Fengpeng S, et al. (2015). A Hybrid Dynamical–Statistical Downscaling Technique. Part II: End-of-Century Warming Projections Predict a New Climate State in the Los Angeles Region. *Journal of Climate*. 28:4618-4636.

⁹ Knowlton, K., et. al. 2009. The 2006 California Heat Wave: Impacts on Hospitalizations and Emergency Department Visits. *Environmental Health Perspectives*, 117(1): pp. 61-67.

¹⁰ Basu, R., Feng, W., and Ostro B. 2008. Characterizing temperature and mortality in nine California counties. *Epidemiology*, 19(1): pp. 138-45.

Figure 10-4: Projected Number of Extreme Heat Days



Note: Business as Usual Scenario (High Emissions), CanESM2 Model (Average)
 Source: CalAdapt

Heat waves, defined as three or more days with temperatures above 90°F, are also projected to occur more frequently by the end of the century. These events are predicated to impact larger areas, last longer, and have higher temperatures.¹¹

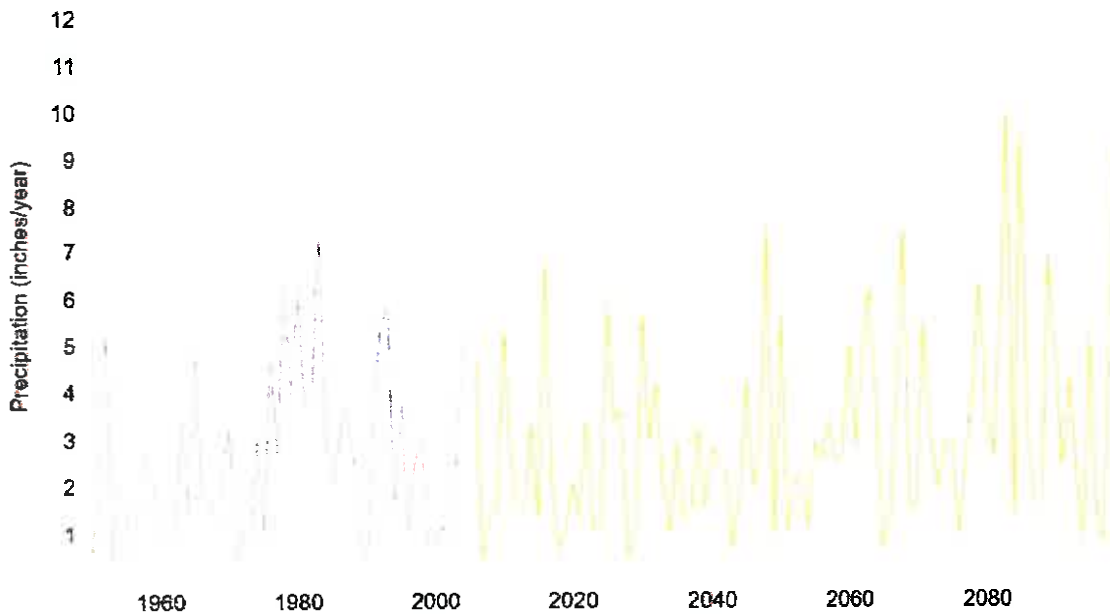
Precipitation

Between 1961 and 2005, the historic annual precipitation mean in the Indio region was approximately 2.8 inches.¹² Annual precipitation, however, varies significantly between years. Overall changes in future precipitation and runoff are highly variable among climate models and relatively uncertain. Some precipitation projections for the region show a slight increase in annual rainfall, others show a slight decrease, and others show no change at all.¹³ During the next century, Indio can expect approximately the same amount of total annual precipitation as it received in the last few decades of the 20th century. Figure 10-5 shows historic and future precipitation levels in Indio, which has fluctuated over time.

¹¹ Gershunov, A., and Guirguis, K. (2012). California heat waves in the present and future. *Geophysical Research Letters*, 39(18), 7.

¹² California Energy Commission. 2017. Cal-Adapt. Available at <http://beta.cal-adapt.org/>.

¹³ Berg, N, et al. 2015. Twenty-Frist Century Precipitation Changes over the Los Angeles Region. *Journal of Climate*. 28: 401 – 421.

Figure 10-5: Average Annual Precipitation

Note: Business as Usual Scenario (High Emissions), CanESM2 Model (Average)
 Source: CalAdapt

In the present-day climate, the region experiences wide swings in precipitation from year-to-year, and this variability is expected to continue under climate change with fluctuations between wet years and dry years.¹⁴ Southern California’s annual variability originates primarily from fluctuations of the biggest storms, with approximately 90% of variability coming from the wettest days.¹⁵ Therefore, drought happens during years missing a few large storms and wet years occur when there are large storms. Figure 10-5 shows the projected annual variability in precipitation, which is relatively consistent with historic observations.¹⁶

Due to anticipated warmer temperatures, more intense periods of rain may lead to an increase in winter runoff, which may impact flood risk. Annual stormwater runoff volume and peak runoff may increase. Increased runoff volume and higher peak flow rates are anticipated to increase flood risk.

¹⁴ Berg, N, et al. (2015). Twenty-First Century Precipitation Changes over the Los Angeles Region. *Journal of Climate*. 28: 401 – 421.

¹⁵ Dettinger, M.D., and Cayan, D.R. (2014) Drought and the California Delta—A matter of extremes: *San Francisco Estuary and Watershed Science*, 12(2).

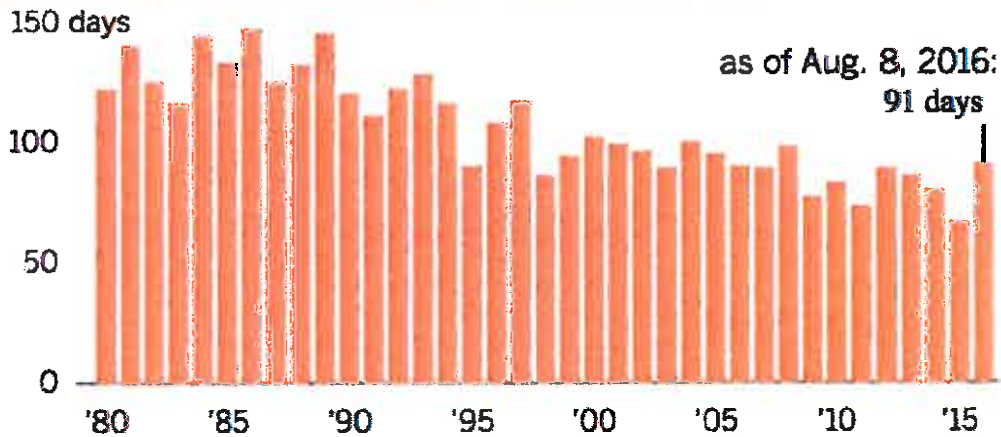
¹⁶ California Energy Commission. (2017). Cal-Adapt. Available at <http://beta.cal-adapt.org/>.

Air Quality and Wildfires

Air quality is expected to worsen with increasing climate change. Air quality is strongly dependent on weather, and climate change is expected to impact air quality through warming temperatures and more frequent episodes of stagnant air. Many strategies that are used to reduce greenhouse gases will also reduce emissions of air pollutants, such as ozone and particulate matter.

Overall, Southern California has the nation’s worst smog and consistently fails to meet federal ozone standards since 1979.¹⁷ The number of days above the ozone standard, however, has been steadily declining since the 1980’s in the South Coast Air Basin, as shown in Figure 10-6. However, in the summer of 2016, Southern California experienced its worst smog since 2008. By August 2016, ozone had exceeded federal standards on 91 days since the beginning of the year, compared to 67 days over the same period the previous year.¹⁸

Figure 10-6: Days above Ozone Standard – South Coast Air Basin

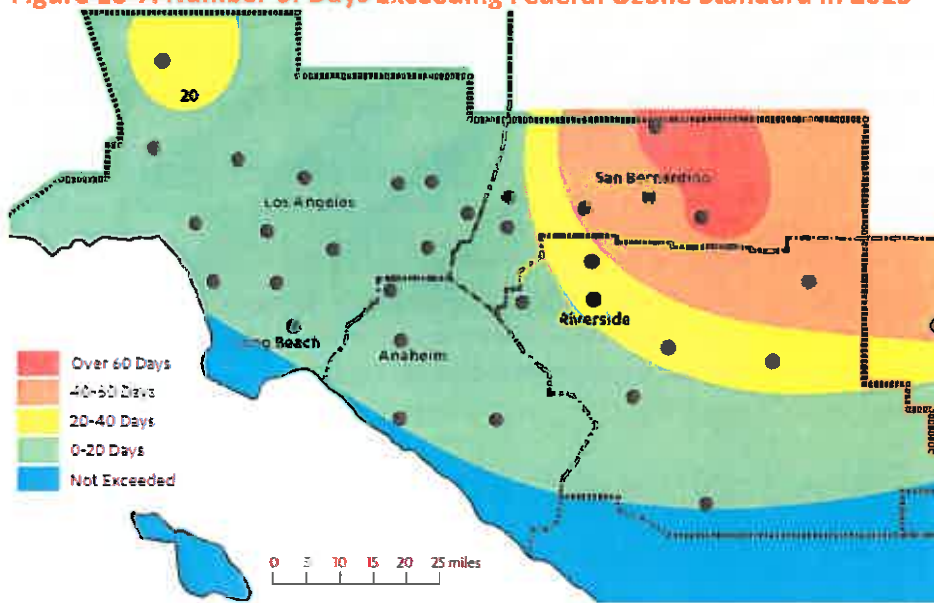


Source: Los Angeles Times

Figure 10-7 shows the number of days exceeding the federal ozone standard in the Los Angeles region and Coachella Valley. It illustrates the significant differences among non-attainment days across the entire region, including more days in Riverside County and the Coachella Valley.¹⁹

¹⁷ CalEPA. 2017. Air Quality and Meteorological Information (AQMIS2). Available at <https://www.arb.ca.gov/aqmis2/display.php?year=2017&report=AREAMYR&o3pa8=SC¶m=OZONE&otvqe=aqd>.
¹⁸ Barboza, T. Aug. 11, 2016. SoCal Hit with Worst Smog in Years as Hot, Stagnant Weather Brings Surge in Hospital Visits. Los Angeles Times. Available at <http://www.latimes.com/local/lanow/la-me-ln-summer-smog-20160805-snap-story.html>.
¹⁹ UCLA Institute of the Environment and Sustainability. 2016. 2015 Environmental Report Card for Los Angeles County.

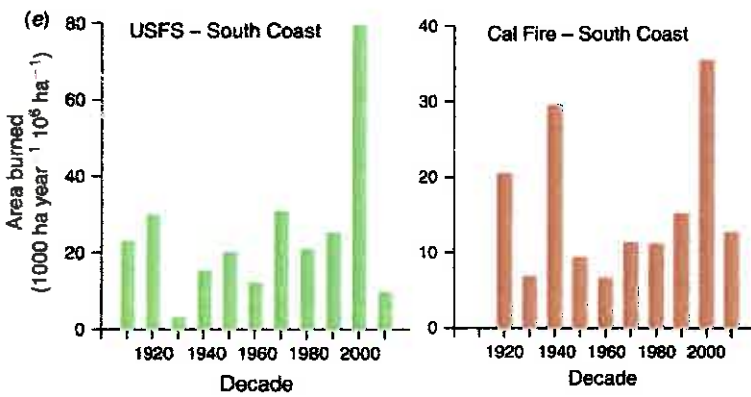
Figure 10-7: Number of Days Exceeding Federal Ozone Standard in 2013



Source: UCLA Institute of the Environment and Sustainability

Wildfires have increased over the Western US and Southern California in recent decades. These changes in wildfire pattern are often attributed to climate change and fire suppression techniques. As shown in Figure 10-8 the South Coast region, which includes Indio, had significant peaks in the area burned in the 1920s, 1940s, 1970s, and 2000s. The South Coast region was among the few areas within the State that had an increase in burned area in recent decades.²⁰

Figure 10-8: Decadal Burning on US Forest Service and Cal Fire Managed Lands in California Climate Divisions



²⁰ Keeley, JE, AD Syphard. (2017). Different historical fire-climate patterns in California. International Journal of Wildland Fire 26(4): 253-268.

While the direct impacts of wildfire may be less of a concern for Indio, wildfires can be a significant source of air quality pollution. Wildfires burning within 50 to 100 miles of a city routinely cause air quality to be 5 to 15 times worse than normal, and often two to three times worse than the worst non-fire day of the year.²¹ Emissions from wildfire can lead to excessive levels of particulate matter, ozone, and volatile organic compounds.²²

Unequal Impacts

While climate change affects everyone in a community, not all people are impacted equally. People of color, immigrants, and lower-income populations experience increased exposure and sensitivity to climate hazards and a reduced capacity to adapt. Cities that begin planning now will have the best options for adapting to climate change. The City of Indio encourages residents to know the risks in their communities and be informed with information about severe heat warnings and other climate related hazards.

The impacts of climate change are already being felt in some places, but they are relatively small at this time. However, the longer cities wait, the greater the costs of these impacts. As Indio begins to plan for and implement their response to climate change, an opportunity exists to create stronger, more equitable communities for everyone. Many of the actions needed to reduce the impacts of climate change will provide additional benefits to the community, including individual climate preparedness, increased public safety, reduced greenhouse gas emissions, and greater economic stability.²³

²¹ Kenward, A, et al. 2013. Wildfires and Air Pollution: The Hidden Health Hazards of Climate Change. Climate Central. Available at <http://assets.climatecentral.org/pdfs/WildfiresAndAirPollution.pdf>.

²² Phuleria, HC, et al. 2005. Air Quality Impacts of the October 2003 Southern California Wildfires. Journal of Geophysical Research. 110(D7).

²³ California Emergency Management Agency and Natural Resource Agency (2012). California Adaptation Planning Guide. http://resources.ca.gov/docs/ciimate/01APG_Planning_for_Adaptive_Communities.pdf

Goals and Policies

Emergency Response

The City will continue to implement best practices to maintain high quality emergency services and response.

Goal SE-1: Police Services. Excellent law enforcement and a reduction in criminal activities and focus on community policing.

SE-1 Policies

- SE-1.1 Service Levels.** Maintain adequate police protection capabilities by retaining the current peace officer staffing positions, forecasting future demand, and providing additional staff, law enforcement equipment and technology acquisition, and facilities when fiscally appropriate and needed.
- SE-1.2 Proactive Policing Strategies.** Collaborate with the Bureau of Justice Assistance (BJA), the SNA Research Corporation, and academic researchers to address crime and quality of life issues through the implementation of best practices in policing.
- SE-1.3 Community Engagement.** Continue to implement community-oriented policing strategies that emphasize community engagement and collaboration.
- SE-1.4 Crime Prevention Through Environmental Design.** Promote Crime Prevention Through Environmental Design (CPTED) concepts, including, but not limited to:
- Controlling access by creating real and perceptual barriers to entry and movement through the use of fences or landscaping to define site boundaries, clearly defined pathways to guide movement, gates or doors to limit access, and signs to define appropriate activities.
 - Maximizing opportunities to see and be seen through the use of lighting, windows, building orientation and location, proper selection of landscaping materials and regular maintenance, furniture arrangements, surveillance equipment, or other security or design measures.
 - Clearly defining ownership and encouraging maintenance of properties through measures such as landscaping, front porches, fencing, variations in paving materials, or other elements to distinguish between private and public spaces. Display signs to establish ownership and keep buildings, yards, gardens, sidewalks, and other features well maintained, clean, and in working order.
- SE-1.5 Neighborhood Watch.** Continue to support the formation of neighborhood watch groups.
- SE-1.6 Effective Programs for At-Risk Programs.** Continue to support programs for at-risk youth and parents of at-risk youth, such as the nationally recognized “The Parent Project” which seeks to prevent and intervene in destructive adolescent behavior, in addition to facilitating youth community outreach events and participation in Youth Court Program where juvenile offenders are tried in front of a jury comprised of their peers.

- SE-1.7 Volunteer and Educational Programs.** Continue to promote volunteer and educational programs to assist police personnel such as the police cadets, Citizens Helping Indio Police (CHIP), and the Police Chaplain Program.
- SE-1.8 Graffiti Abatement.** Continue the robust graffiti prevention and removal efforts through the City of Indio's Graffiti Abatement Team which is managed by the Indio Police Department. The Team removes graffiti from public property (including parks, street signs, sidewalks, etc.) or property adjacent to public rights-of-way.
- SE-1.9 Funding.** Assess the adequacy of the current funding structure for police services and facilities and continue to monitor and assess the opportunities for additional funding sources such as special event reimbursement fees, research and application for state and federal grants, development impact fees, a public safety tax for facility construction, and general fund increases.

Community Emergency Preparedness

The City of Indio will work with local agencies and organizations to promote resilience and disaster preparedness.

Goal SE-2: Emergency Preparedness. Provide City leadership within the region by promoting a collaborative environment that sustains maximum resilience to emergencies and disasters.

SE-2 Policies

- SE-2.1 Local Hazard Mitigation Plan.** Maintain the City's Local Hazard Mitigation Plan (LHMP) as an integrated component of the General Plan, in coordination with Riverside County and other participating jurisdictions. After each regular LHMP update, update the City's Safety Element to ensure consistency.
- SE-2.2 Emergency Operations Plan.** Ensure that the City's Emergency Operations Plan is compatible with Federal, State and local emergency requirements, review for possible updates every 2-3 years, as directed by FEMA Best Practices.
- SE-2.3 Climate Change.** When updating hazard mitigation and emergency management plans, incorporate climate change hazards, vulnerabilities, and risks into the analysis.
- SE-2.4 Hazard Profile.** Work to minimize the frequency, severity, and probability of future hazard events in the City by taking actions that prepare and mitigate those hazards before they occur.
- SE-2.5 City Ordinances:** Ensure the effectiveness of the City's Ordinances in addressing requirements for emergency access and evacuation in new and existing development.
- SE-2.6 CERT.** Continue to promote citizen-based disaster preparedness and emergency response through Riverside County's Community Emergency Response Team (CERT) training and certifications to engage the community to be self-reliant and prepared to take care of themselves and others. Encourage members from IPD and FD to become certified CERT instructors, and also volunteer corps can be CERT instructors also.

SE-2.7 Public Awareness. Promote public awareness of the City’s emergency preparedness and response techniques by:

- Distributing the Emergency Operations Plan (EOP) to business associations, community groups, schools, hospitals, and other community groups each time an updated version is completed; and
- Maintaining a current version of the EOP on the City’s website.

SE-2.8 Response Times. Periodically update emergency response times and other indicator measurements, to monitor progress made in implementation of the General Plan and make adjustments to implementation measures/programs as necessary to improve the City’s effectiveness.

SE-2.9 Interdepartmental Coordination. Work with City applicable departments to:

- Conduct mock emergency drills to test the effectiveness of emergency response activities.
- Review and update designated evacuation routes as needed.
- Ensure that public facilities and services are targeted towards the most vulnerable populations and that critical facilities are not sited in geologic or flood hazard zones.
- Remain up-to-date on mandatory emergency management training for all departments, as necessary.

Fire

Goal SE-3: Fire Safety. A community safe from the risk of fire and with appropriate fire response standards.

SE-3 Policies

SE-3.1 Compliance. Comply with the National Fire Protection Association (NFPA) 1710 and Riverside County Fire response standard of arriving to fire and medical emergency incidents within a four (4) minute drive time.

SE-3.2 Water Service and Pressure. Ensure that sufficient water service and pressure is available throughout the City for firefighting purposes, including continuing to require new development to provide necessary water mains, fire hydrants, and access for emergency vehicles and personnel.

SE-3.3 Brush and Weed Control. Maintain and enforce standards for weed and brush abatement and establish clearances around structures to minimize fire hazard risk.

SE-3.4 Fire-Resistant Materials. Require the use of fire-resistant building construction materials to reduce the hazard of structure fires, within the developed areas of the City and at the urban-wildland interface.

SE-3.5 Fire Retrofits. Encourage owners of non-sprinklered high-occupancy structures to retrofit their buildings to include internal sprinklers.

SE-3.6 New Growth. Address Indio’s existing and future fire service needs by planning and funding infrastructure to support the City’s growth and continuing to develop and staff new fire stations

when and where they are needed to meet NFPA and County Fire response time standards. Ensure new fire stations are not located in geologic or flood hazard zones.

- SE-3.7 Response Adequacy.** Ensure, to the maximum extent possible, that fire services, such as firefighting equipment and personnel, infrastructure and response times, are adequate for all sections of the City. To that end, continue to regularly evaluate specific fire hazard areas, and adopt reasonable safety standards, such as adequacy of nearby water supplies, fire-retardant roofing materials, fire-equipment accessible routes, clarity of addresses, street signage and street maintenance, and fire-hydrant inspection and maintenance.
- SE-3.8 Development Applications.** Continue to review development applications for consistency with applicable fire and building code regulations, including emergency access/evacuation routes.
- SE-3.9 Partnerships.** Continue to work with the Riverside County Fire Department to utilize the Cooperative Integrated Regional Fire Protection System to fulfill additional resource needs and increase service capabilities during major emergency events.
- SE-3.10 Funding.** Continue to assess the adequacy of the current funding structure for fire and emergency services and research and apply for additional funding sources.
- SE-3.11 Public Education.** Inform the public about fire safety and fire prevention measures by:
- Holding public workshops or speaking engagements at schools, community centers, business associations, and other various groups;
 - Posting information on the City's website and social media pages;
 - Issuing press releases;
 - Distributing emergency preparedness information and handouts at City Hall, community centers, the library, and at community events;
 - Involving the public in mock fire response drills;
 - Continuing to train volunteer firefighters; and
 - Continuing to provide Community Emergency Response Training.

Seismic Hazards

Seismic Hazards have a high probability of occurrence in Indio with high severity of damage. This goal seeks to ensure better responsiveness, minimize risk of property damage and personal injury, and require seismically-compliant buildings and improvements.

Goal SE-4: Seismic Hazards. A community that is minimally affected and less vulnerable to earthquakes and seismic hazards.

SE-4 Policies

- SE-4.1 Development Plan Review.** Require all new structures to be designed in accordance with the most recent California Building Code adopted by City Council, including the provisions regarding seismic loads, lateral forces and grading and not built across the trace of an active fault.
- SE-4.2 Technical Reports.** Require submittal of applicable geotechnical reports prepared by qualified professionals as part of the development review process.

- SE-4.3 Liquefaction.** Require liquefaction assessment studies be conducted for all projects proposed in areas identified as potentially susceptible to liquefaction. In areas where geotechnical testing shows the sediments are susceptible to liquefaction, require the implementation of mitigation measures as a condition of approval. Liquefaction mitigation measures shall be applied to all habitable structures, bridges, roadways, major utility lines, and park improvements to be built in these areas. Work with insurers to require additional insurance coverage in liquefaction areas.
- SE-4.4 Information and Education.** Encourage earthquake preparedness within the community through early and clear information and education so the community avoids and/or is prepared for seismic and geologic hazards. Encourage participation in The Great ShakeOut, an annual earthquake drill in California that Indio residents are encouraged to participate in.
- SE-4.5 Critical Facilities Planning.** When planning for new fire stations or other critical facilities, review hazard maps to ensure that they are not sited in geologic or flood hazard zones and employ critical infrastructure design and building standards to enable City operations to continue after an earthquake.
- SE-4.6 Inventory of Unreinforced Structures.** Periodically review and update the City's inventory of unreinforced masonry buildings, existing multifamily housing constructed before 1971, and other structures that may result in unsafe conditions during seismic events. Any historic landmarks discovered through the inventory process shall be added to the City's inventory of historic places.
- SE-4.7 Seismic Maps.** Maintain an updated Seismic Hazards Map, periodically consult with the California Geological Survey (CGS) Seismic Hazards Zonation Map and coordinate information with geotechnical reports filed at the City to ensure the latest information is available to the City.

Climate Change and Community Resilience

Climate change is anticipated to amplify existing natural hazards. This goal aims to establish a framework for Indio to begin addressing the impacts of climate change.

Goal SE-5: Community Resilience. A community that is prepared for the potential impacts of climate change.

SE-5 Policies

- SE-5.1 Regional Partnerships.** Establish partnerships with State, Federal, regional, and local agencies to collaborate and better understand the regional impacts of climate change, and to develop multijurisdictional solutions.
- SE-5.2 Climate Change Research.** Integrate climate change research and adaptation planning into City operations, services, and public infrastructure development, including capital improvements.
- SE-5.3 Cooling Centers.** Establish cooling centers to reduce Indio resident's vulnerability to extreme heat events and severe storms.
- SE-5.4 Backup Power.** Support critical facilities, such as schools, hospitals, and cooling centers to operate on micro-grids, which use various redundant backup systems including generator power, solar, and wind turbine power sources.

SE-5.5 Neighborhood and Building Cooling. Encourage new development and redevelopment to take steps to reduce the impacts of extreme heat events, including:

- Protect the City's healthy trees and plant new ones to provide shade, increase carbon sequestration and purify the air.
- Shade public parks and open spaces, including bus shelters.
- Support residential energy efficiency and weatherization programs.
- Design buildings to use less cooling through passive heat and cooling techniques.

SE-5.6 Reduced Water Supplies. When reviewing development proposals, consider the possibility of constrained future water supplies and require enhanced water conservation measures.

- Ensure compliance with the landscape conservation ordinance.
- Encourage the use of water conservation measures in new development beyond current requirements.
- Encourage the use of sustainable landscaping techniques.
- Support recycled water use.

SE-5.7 Communications and Outreach. Continue to work with the Riverside County Public Health Department and County of Riverside Emergency Management Department to establish social networks and website updates to distribute information on climate change impacts to vulnerable populations including actions they can take to reduce exposure to unhealthy conditions.

SE-5.8 Equitable Distribution of Resources. Prioritize programs that ensure the benefits of climate action programs are fairly distributed and prioritized to those most in need, particularly populations most likely to be impacted by climate change.

SE-5.9 Funding Opportunities. Pursue climate change grant funding opportunities when appropriate.

Flooding

This goal aims to minimize community risk from stormwater runoff, flooding or inundation hazards, and minimize community expenditure of funds, and emergency personnel resources.

Goal SE-6: Flood Hazards. A community that is minimally disrupted by flooding and inundation hazards.

SE-6 Policies

SE-6.1 Flood Hazard Enforcement. Restrict development in Special Flood Hazard Areas. Require mitigation measures which may include (but are not limited to) the design of onsite drainage systems connected to the Coachella Valley Stormwater Channel, keeping surface waters within the project area, grading of the sites so that runoff does not affect adjacent properties, and building structures so they are elevated above the anticipated flood levels.

SE-6.2 Flood Mitigation in Repetitive-Flooding Areas with Existing Development. Prioritize hydrological studies of areas that flood repeatedly during storms and require feasible engineering solutions to mitigate these sites. Prohibit any additions or reconstruction of structures damaged by flooding, unless the structure is relocated to a safer area or can be demonstrated that the proposed project and its occupants can be protected from future,

recurrent flood damage by implementing mitigation measures not present in the original, damaged structure.

- SE-6.3 Hydrological Studies in New Development.** Require new development proposals to include as a condition of approval, hydrological and hydraulic studies prepared by a state-certified engineer with expertise in these kinds of studies, that assess the impact the new development will have on the flooding potential of existing development down-gradient. The studies shall provide mitigation measures to reduce this impact to an acceptable level.
- SE-6.4 New Critical Facilities.** Limit the future development of critical facilities including, but not limited to, hospitals and health care facilities, emergency shelters, fire stations, emergency command centers, and emergency communications facilities within the boundaries of the 100-year flood plain.
- SE-6.5 Existing Critical Facilities.** Encourage critical facilities to implement feasible mitigation measures that ensure the building will not flood during a 100-year flood event to greatest extend practical. Also encourage ingress/egress of facility also follows mitigation measures.
- SE-6.6 Storm Drainage Facilities and Stormwater Runoff.** Maintain, develop and improve where needed, the storm drain facilities (including bridges and other stormwater channel crossings) with an emphasis on those areas in the City that flood repeatedly. Promote the use of bio-swales, tree wells, green roofs, and other infiltration mechanisms to reduce the volume and velocity of stormwater runoff.
- SE-6.7 Hazardous waste in stormwater.** Support Indio residents and businesses to properly dispose of hazardous waste by continuing to host bi-annual collection events.
- SE-6.8 Floodplain Development.** Promote the use of floodplains as parks, nature trails, equestrian parks, golf courses or other types of recreational facilities that can withstand periodic inundation. In the planned build-out of the City, create an atmosphere of working with nature and the natural processes characteristic of the arid environment.
- SE-6.9 Flood Barriers.** Minimize construction of flood barriers within the 100-year flood plain which would divert flood water or increase flooding in other areas.
- SE-6.10 Coordination.** Work with the Coachella Valley Water District (CVWD), the Regional Floodplain Administrator, the Indio Water Authority (IWA), and federal agencies, where applicable, to limit the potential for inundation by levee or water tank failure, or seismically induced inundation.
- SE-6.11 Disaster Response Plan.** Require all essential and critical facilities, including but not limited to essential City offices and buildings, medical facilities, schools, childcare centers and nursing homes in or within 200 feet of Flood Zones A and X, to develop disaster response and evacuation plans that address the actions to be taken in the event of storm flooding or inundation due to catastrophic failure of a water reservoir or other water retention facilities such as the All American Canal, the Eastside Dike and levees of the Coachella Valley Stormwater Channel. Encourage the use of technology to identify flood-prone areas and to warn residents and motorists of impending flood hazards.

- SE-6.12 **Residential Response Plan.** Require new residential associations over one hundred homes to establish a response plan and practice the plan regularly.
- SE-6.13 **Flood Insurance.** Encourage property owners and residents to purchase flood insurance for areas outside of the FEMA-mapped 100-year flood zones, especially in those areas that have experienced flooding in the past.
- SE-6.14 **Land Use and Flood Hazard Maps.** In coordination with the Public Works Department, annually review the City's Land Use and Flood Hazard Maps to ensure that they accurately reflect areas recognized by the Federal Emergency Management Agency (FEMA) as being subject to flooding.
- SE-6.15 **Designing for Changing Precipitation Patterns.** Periodically evaluate stormwater control strategies and systems for sensitivity to changes in precipitation regimes and consider adjusting those strategies to accommodate future precipitation regimes.

Hazardous Material

The City will work to protect the community from hazardous material contamination.

Goal SE-7: Hazardous Materials Management. A healthier and safer community with reduced potential for hazardous materials contamination

SE-7 Policies

- SE-7.1 **Enforcement Actions.** Require all users, generators, and transporters of hazardous materials and wastes to identify the materials they store, use or transport, in accordance with disclosure laws, to have adequate spill clean up kits while transporting, and to have a clean up response plan with contact phone numbers.
- SE-7.2 **Hazardous Materials Transport Routes.** Require submittal of a transportation of hazardous materials plan to address possible impacted facilities along these routes and identify emergency response actions that can be implemented in case of an accident. Limit transportation routes to areas away from critical facilities and infrastructure and not over bridges if possible.
- SE-7.3 **Hazardous Materials Siting.** Prohibit the placement of proposed new facilities that will be involved in the production, use, storage, transport or disposal of hazardous materials near existing land uses that may be adversely affected by such activities. Conversely, prohibit the development of new sensitive facilities (like schools, child-care centers, nursing homes, senior housing, etc.) near existing sites that use, store or generate hazardous materials.
- SE-7.4 **Site Remediation.** Encourage and facilitate the adequate and timely cleanup of existing and future contaminated sites and the compatibility of future land uses.
- SE-7.5 **Agency Coordination.** Coordinate with the Riverside County Environmental Health Department to maintain and periodically update an inventory of permitted hazardous materials and clean-up sites within the City and monitor future uses at those locations. Work with other City departments to promote programs to reduce hazardous materials and promote non-toxic, non-hazardous alternatives in City operations.
- SE-7.6 **Hazardous Mitigation Training.** Establish clear policies and protocol for action in the event of a hazardous contamination. Recommend and offer trainings to private sector companies.

Implementation Actions

The table below identifies programs, policy updates, planning efforts, coordination efforts, and other actions that will help implement the General Plan’s Safety policies. Programs are consistent with this chapter’s goals and policies.

Table 10-2: Safety Programs

	DESCRIPTION	PRIORITY	TIME FRAME	RESPONSIBILITY
1	Plan Consistency. Following adoption of the General Plan, review the existing hazard mitigation and emergency operations plans and make changes to ensure consistency with the General Plan.	High	Ongoing	Emergency Services Coordinator
2	California Building Codes. As new versions of the California Building Standards Code (CCR Title 24, published triennially) are released, adopt and enforce the most recent codes. Specifically, to minimize damage from earthquakes and other geologic activity, implement the most recent State and seismic requirements for structural design of new development and redevelopment.	Medium	Ongoing	Planning, Public Works
3	Develop City Standards and Guidelines. The City shall develop standards and guidelines and require their use in new construction to provide the greatest possible protection for human life and property in areas where there is a high risk of seismic or geologic hazard occurrence. This could include requirements for geotechnical reports and seismic safety plans and setbacks for new construction, among others.	Medium	Short	Planning
4	Maintain GIS Database. Maintain the City’s Geographic Information System (GIS) database to assist hazard planning, including: <ul style="list-style-type: none"> • Critical facilities • Vulnerable populations • Infrastructure 	High	Ongoing	Planning, Public Works
5	Critical Facility Insurance Coverage. Document current City insurance coverage on critical City facilities. Collect all costs associated with mitigation from a disaster and operating costs as required by Federal funding eligibility.	High	Short	Emergency Services Coordinator
5	Residential Disaster Response Plans. Establish a program that requires all residential associations over 100 homes to establish a disaster response plan and practice the plan regularly. Establish a disaster	Low	Ongoing	Emergency Services Coordinator, Planning

	response drill/exercise schedule requirement and practice annually.			
6	Emergency Management. Establish an ordinance for anti-price gouging, on call debris management contractors, and pre-identified mass care shelter locations.	Low	Short	Emergency Services Coordinator
7	CERT Newsletter. Regularly publish a CERT newsletter.	Low	Short	Emergency Services Coordinator
8	Reduce Fire Hazards. Amend the Municipal Code to include weed abatement/brush management regulations to reduce fire hazards and ensure adequate coordination between planning and fire issues.	Medium	Short	Planning, Emergency Services Coordinator
9	Hazard Impact Assessment. Hire a consultant to conduct an Hazards Impact Assessment for City of Indio that will identify losses to revenue, and recovery time objectives for businesses and the City.	Medium	Medium	Emergency Services Coordinator
10	Disaster Mitigation Funding. Consider applying for Mitigation grant funding opportunities before a disaster (Stafford Act Section 404) and prepare as much as possible for mitigation projects after a disaster (Stafford Act Section 406). Typically private property is not eligible, but there may be other funding opportunities for HOAs, etc.	Medium	Ongoing	Emergency Services Coordinator
11	Hazardous Mitigation Training Course. Implement an 8-hour hazardous mitigation basic training course requirement for new hires of the City of Indio.	Low	Short	Emergency Services Coordinator
12	Climate Vulnerability Assessment. Complete a climate vulnerability assessment consistent with the requirements of SB 379 at the time of the next Housing Element update.	High	Short	Planning, Emergency Services Coordinator

 **10** **Indio**
other Desert
Cities


Pal





chapter eleven

NOISE

The purpose of the Noise Element is to minimize exposure to excessive noise which can adversely affect physical and psychological well-being, property values, the environment, and quality of life. As required by State law, local governments are required to quantify noise levels and exposure through analysis and noise modeling. This element evaluates community noise levels and establishes a land use pattern that is compatible with current and future noise sources. California law requires that a general plan include elements (or chapters) specifically addressing noise. This element was prepared to meet these requirements (Government Code Section 65302(f)). The Noise Element must identify and appraise noise problems in the community from a variety of sources, establish a pattern of land use that minimizes exposure of residents to excessive noise, and include possible solutions to address existing and foreseeable noise problems.

Key Considerations and Strategies

Noise is usually defined as an unwanted sound. Excessive levels of noise can interfere with sleep, work, and quality of life. It can even cause physiological or psychological damage. The impact of noise is not a function of loudness alone. The time of day when noise occurs and the duration of the noise are also important. In addition, most noise that lasts for more than a few seconds is variable in its intensity. At excessive levels, people typically perceive noise as being intrusive, annoying, and undesirable.

Noise-sensitive receptors are land uses that tend to be more impacted by noise than others. Examples of such land uses include residential dwellings, hotels, hospitals, nursing homes, educational facilities, libraries, and biological open space. Excessive noise can interfere with human activities in these locations, such as talking, reading, and sleeping. Animal species and their habitats may also be affected by noise, especially during their breeding season.

Many potential noise impacts can be addressed through the design of our buildings and public spaces. How buildings are designed, how land uses are arranged, and how our roads are designed and operated all contribute to the ambient noise levels in our community. The most efficient and effective way to minimize noise impacts is at the time of project design through the use of strategies that reduce the noise levels generated such as through the use of mufflers on stationary equipment and reducing traffic speeds, protect receptors from noise such as through the use of sound walls along roads, managing the time frame of noise generation such as through the use of operating hours, and reducing noise exposure by separating activities through the use of setbacks.



Traffic in Indio contributes to noise levels.

Context

Transportation, including roadways, rail, and the Bermuda Dunes Airport, represents the most dominant source of noise in Indio. Figure 11-1 identifies the existing roadway transportation noise contours throughout Indio. The most significant roadway noise is generated from motor vehicles travelling along I-10, SR-86, and Highway 111. Other major sources of transportation noise are railroad activity from the Union Pacific rail line that runs adjacent to Indio Boulevard and airport activity from the Bermuda Dunes Airport. The Bermuda Dunes Airport Noise Contours are shown Figure 11-2.

Festivals and special events also generate a significant amount of noise in Indio. The City established the Major Music Festival Overlay Zone around the Festival District in order to monitor and adjust noise levels during major festivals. Other noise sources in Indio are manufacturing and industrial operations, agricultural operations, air conditioning and other mechanical equipment, landscaping equipment, and human speech.

Noise and Land Use Compatibility Matrix

Indio has established noise / land use compatibility guidelines. The noise / land use compatibility guidelines are a planning tool that establishes criteria for the acceptable total noise levels to which land uses are exposed. Proposed developments are assessed for conformance with the noise land use compatibility guidelines.

Table 11-1 displays these guidelines, which are based on Community Noise Equivalent Level (CNEL). CNEL averages noise levels over a 24-hour period and is weighted to recognize that sounds appear louder during the quiet evening and nighttime hours when ambient noise levels are lower. The CNEL scale identifies the acceptable ranges of noise for a various land uses ranging from single-family residential to industrial and manufacturing uses. As shown in Table 11-1, acceptable noise levels increase as the sensitivity of the land use decreases.

Once land uses are established, noise levels are regulated through the City's noise ordinance (Chapter 95C of the Municipal Code), which establishes hourly noise level limits and enforcement procedures to restrict noise from individual noise generators.

Coordination of the guidelines and the noise ordinance is essential to reducing noise impacts. The guidelines reduce the burden of enforcement through preemptive measures and also address noise sources, such as traffic noise, which cannot be restricted after land uses are established. The noise ordinance provides continued monitoring to ensure actual and planned noise levels are consistent. This includes regulation of nuisance noise sources that cannot be planned for and noise that would be considered acceptable during daytime hours and disruptive during nighttime hours.

Table 11-1: Noise Compatibility Guidelines




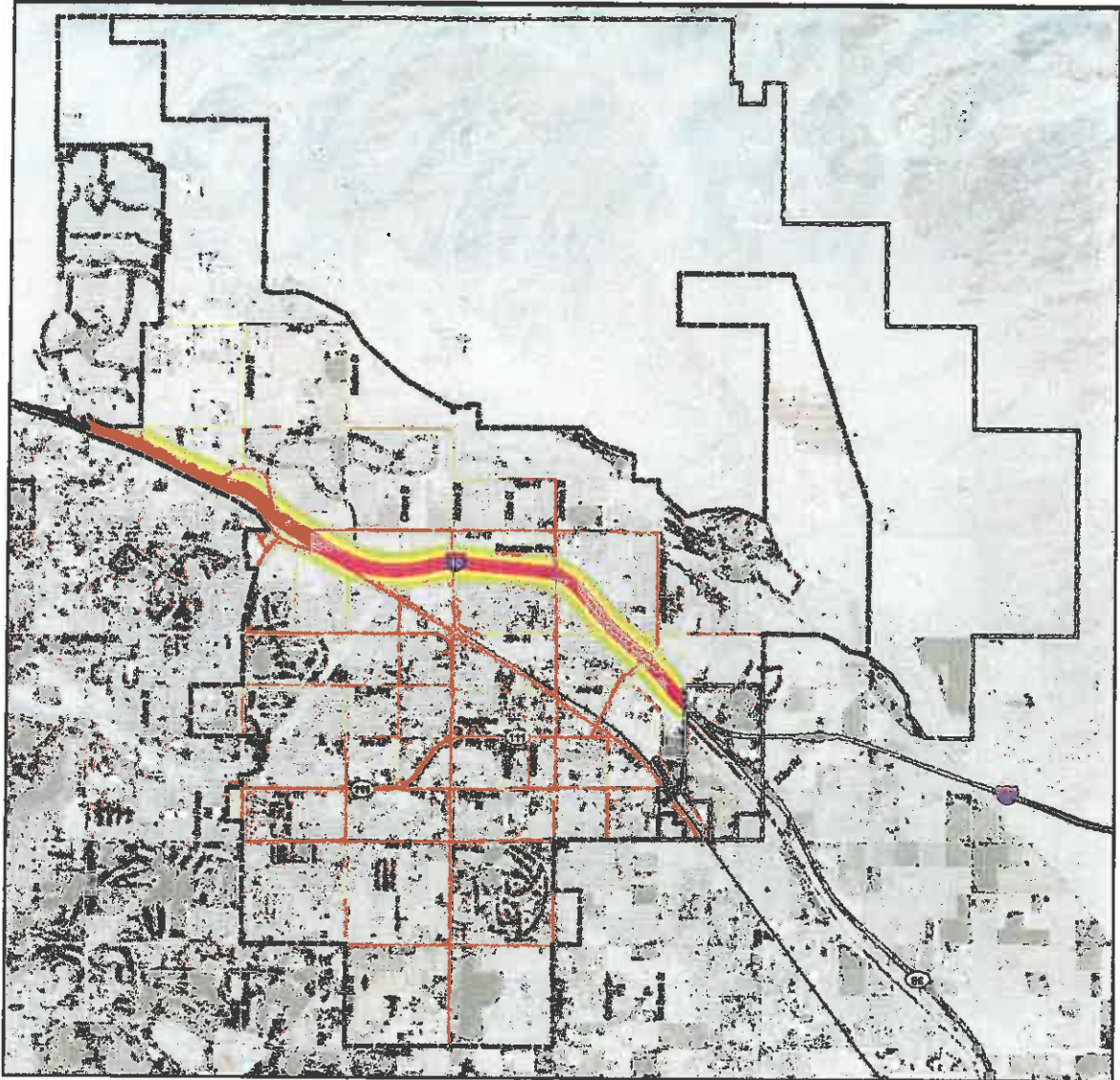
Land Use Category		Exterior Noise Level (CNEL)						
		50	55	60	65	70	75	80
A	Residential – single family residences, mobile homes, senior housing, convalescent homes							
B	Residential – multi-family residences, mixed-use (commercial/residential)							
C	Transient lodging – motels, hotels, resorts							
D*	Schools, churches, hospitals, nursing homes, child care facilities							
E*	Passive recreational parks, nature preserves, contemplative spaces, cemeteries							
F*	Active parks, golf courses, athletic fields, outdoor spectator sports, water recreation							
G*	Office/professional, government, medical/dental, commercial, retail, laboratories							
H*	Industrial, manufacturing, utilities, agriculture, mining, stables, ranching, warehouse, maintenance/repair							
	ACCEPTABLE – Specified land use is satisfactory, based upon the assumption that any buildings involved are of normal construction, without any special noise insulation requirements.							
	CONDITIONALLY ACCEPTABLE – New construction or development should be undertaken only after a detailed noise analysis is conducted to determine if noise reduction measures are necessary to achieve acceptable levels for land use. Criteria for determining exterior and interior noise levels are listed in Table N-2, Noise Standards. If a project cannot mitigate noise to a level deemed Acceptable, the appropriate county decision-maker must determine that mitigation has been provided to the greatest extent practicable or that extraordinary circumstances exist.							
	UNACCEPTABLE – New construction or development shall not be undertaken.							

Figure 11-1: Existing Transportation Noise Contours (Non-Airport)



Existing Noise Contours

- City Boundary
- Sphere of Influence
- Roads
- Railroads
- Highways

Existing Noise Contours

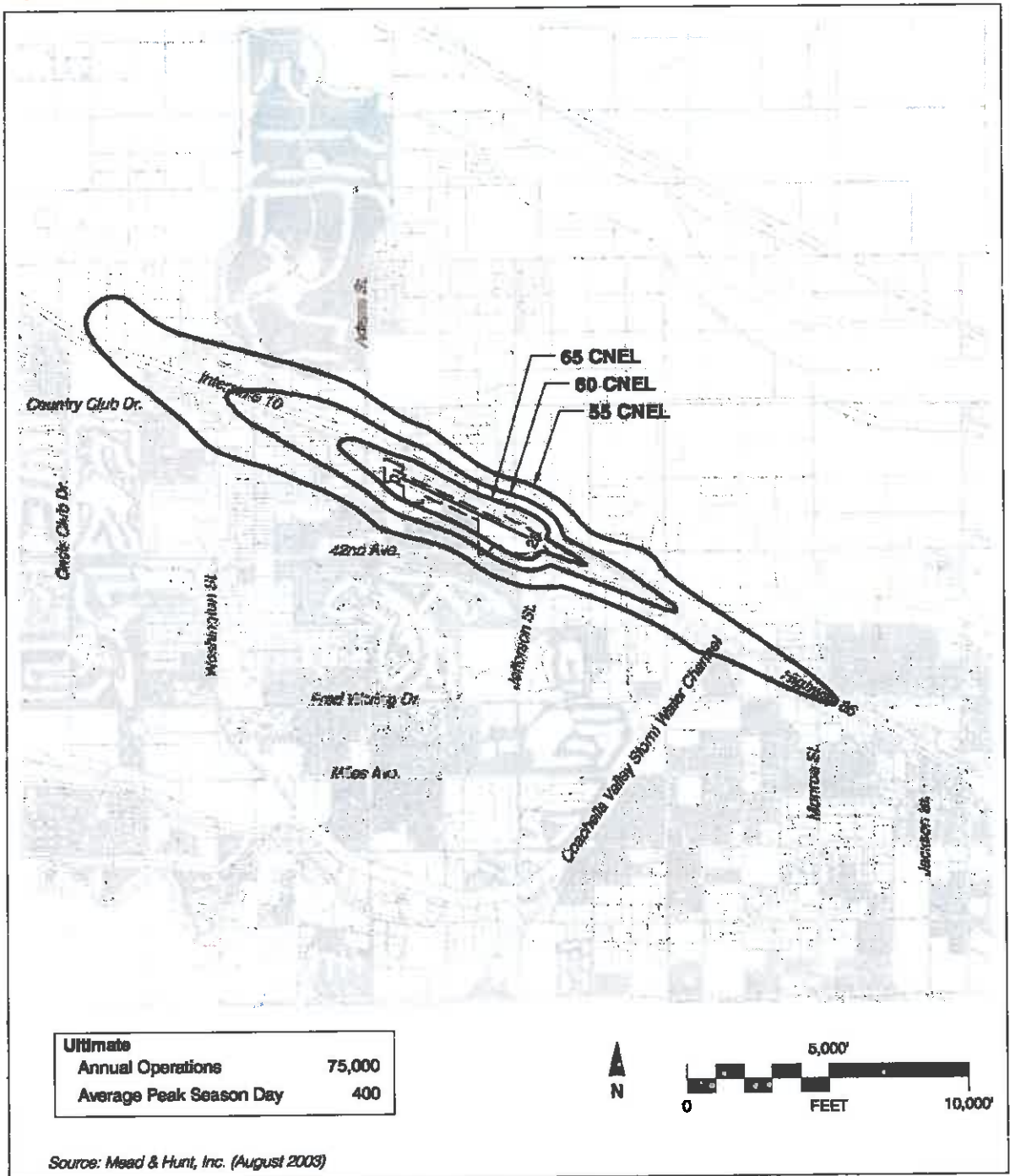
- 60-65 CNEL
- 65-70 CNEL
- 70+ CNEL



Source: Riverside County AFD (City Boundary 2012, 2017)
 Riverside County TMA (Roads, Railroads, Highways) Riverside County (2017)

The information on this map was derived from various digital databases, scanned maps, CAD files taken in the creation of this map but is provided "as is". PDC cannot accept any responsibility for any errors, omissions, or positional accuracy, and therefore, there are no warranties which accompany this product. Users are cautioned to field verify information on this product before making any decisions.

Figure 11-2: Bermuda Dunes Airport Noise Contours

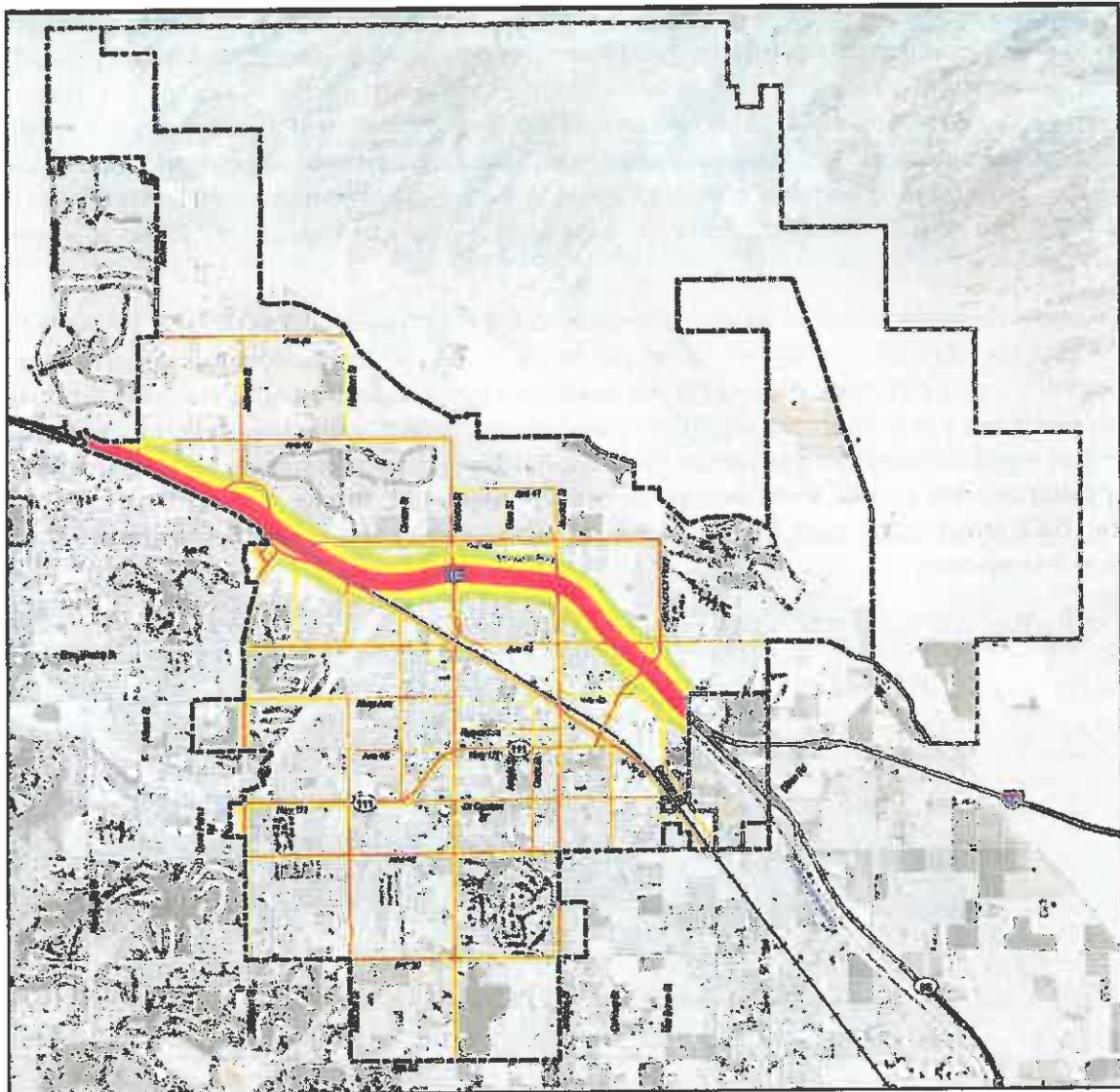


Future Noise Conditions






Future conditions from most noise sources would remain substantially similar to existing conditions. Railway operation and aircraft overflights would have limited increases in operational frequency. As Indio grows, temporary noise sources, such as construction activities and special events, may increase in frequency; however, there would be no fundamental change in the nature of these sources and existing regulation under the noise ordinance would continue. Stationary sources of noise associated with land uses, such as landscaping equipment, air conditioning units, and other mechanical equipment would be associated with new developments; however, these sources would be substantially similar to existing uses.

Indio's most dominant source of noise, traffic noise, would increase with future development in Indio. Noise from freeway segments adjacent to Indio including I-10 and SR-86 would increase by an average 3 dB(A). Noise from local surface streets including major thoroughfares and minor collectors would increase by an average of 2 to 3 dB(A). Although traffic noise increases would occur throughout the community, increases would be exaggerated on certain roadways. Roadways subject to a readily perceptible (5 dB[A] or greater) increase in noise levels include segments of Avenue 38, Avenue 40, Burr Street, Madison Street, Oasis Street, Dillon Road, Avenue 44, Avenue 48, Avenue 50, and Avenue 52. Figure 11-3 shows future noise contours.



Figure 11-3: Projected Future Transportation Noise Contours (Non-Airport)

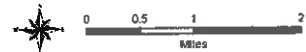


Projected Noise Contours

-  City Boundary
-  Sphere of Influence
-  Roads
-  Railroads
-  Highways

Projected Noise Contours

-  60-65 CNEL
-  65-70 CNEL
-  70+ CNEL



Source: Riverside County LACU, City Downtown, 2012; 9/6/2012
 Riverside County TIRRA (Roads, Railroads, Highways) Riverside County
 Urban

The information on this map was derived from various digital
 databases, sources, and data. While every effort has been made to
 ensure the accuracy of the information, the user assumes all
 responsibility for any errors, omissions, or inaccuracies. There are no
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Goals and Policies

Noise Compatibility

Establishing noise/land use compatibility guidelines helps to prevent and/or minimize adverse or undesirable noise impacts. Noise can cause stress, disrupt sleep, and cause health and auditory problems. For the well-being of the community, land use planning is important to avoid excessive noise levels.

Goal NE-1: Land Use Compatibility. A City where noise exposure is minimized for those living, working, and visiting the community.

NE-1 Policies

- NE-1.1 Sensitive Receptors.** Protect noise-sensitive uses, such as residences, schools, health care facilities, hotels, libraries, and churches, from excessive noise levels through land use capability / adjacency, build design, and noise ordinance enforcement.
- NE-1.2 Noise Compatibility.** Apply the Noise Compatibility Matrix, shown in Table 11-1, as a guide for planning and development decisions. The City will require projects involving new development or modifications to existing development to implement mitigation measures, where necessary, to reduce noise levels to at least the normally compatible range shown in the City's Noise Compatibility Matrix shown in Table 11-1. Mitigation measures should focus on architectural features and building design and construction, rather than site design features, such as excessive setbacks, berms, and sound walls, to maintain compatibility with adjacent and surrounding uses.
- NE-1.3 Airport Land Use Planning.** Implement all applicable noise-related policies contained in the Bermuda Dunes Airport Land Use Plan.
- NE-1.4 Major Music Festival Overlay Zone.** Ensure implementation, monitoring, and enforcement of noise standards within the Major Music Festival Overlay Zone.
- NE-1.5 Special Events.** In conjunction with permitting for special events, such as concerts and festivals, permit variances to allow such uses to temporarily exceed the maximum allowable decibel level while minimizing noise impacts to surrounding uses to the extent feasible.
- NE-1.6 Limit on Hours of Operation.** Limit delivery or service hours for stores and businesses with loading areas, docks, or trash bins that front, side, border, or gain access on driveways next to residential and other noise sensitive areas, such as residences, schools, hospitals, religious meeting spaces, and recreation areas.
- NE-1.7 Land Use and Community Design.** Prioritize the building design and character policies in the Land Use and Community Character Element over those in the Noise Element to ensure that new development meets the design vision of the city. This policy will not apply when noise levels are clearly in the incompatible range as shown in the City's Noise Compatibility Matrix shown in Table 11-1.

Noise Sources

The City will implement noise reducing measures, such as appropriate setbacks and/or noise barriers to minimize impacts to adjacent land uses from mobile and stationary sources.

Goal NE-2: Mobile Noise Sources. A City with minimal mobile source-generated noise levels.

NE-2 Policies

NE-2.1 Freeway Noise. Work with Caltrans and the Federal Highway Administration to reduce noise impacts to sensitive receptors along I-10.

NE-2.2 Truck Routes. Regulate traffic flow to enforce speed limits to reduce traffic noise. Periodically evaluate and enforce established truck and bus routes to avoid noise impacts on sensitive receptors.

NE-2.3 Railway Noise. Ensure that noise and vibration from rail lines is taken into account during the land use planning and site development processes.

NE-2.4 Roadway Noise. Implement the policies listed under Goal 1 to reduce the impacts of roadway noise on noise-sensitive receptors where roadway noise exceeds the normally compatible range shown in the City's Noise Compatibility Matrix shown in Table 11-1.

NE-2.5 Traffic Calming. Require the use of traffic calming measures such as reduced speed limits or roadway design features to reduce noise levels where roadway noise exceeds the normally compatible range shown in the City's Noise Compatibility Matrix shown in Table 11-1.

NE-2.6 Noise-reducing Paving. Encourage the use of noise-reducing paving materials, such as open-grade or rubberized asphalt, for public and private road surfacing projects in proximity to existing and proposed residential land uses.

NE-2.7 City Fleet. Consider the noise effects of City purchases and or leases of vehicles and other noise generating equipment. Take reasonable and feasible actions to reduce the noise generated from City-owned or leased vehicles and equipment, where possible.

Goal NE-3: Stationary Noise Sources. A City with minimal stationary source-generated noise levels.

NE-3 Policies

NE-3.1 Noise Ordinance. Minimize noise conflicts between neighboring properties through enforcement of applicable regulations, such as the City's Noise Control Ordinance.

NE-3.2 Noise Complaints. Respond timely to noise complaints and conduct field monitoring compliance checks to regulate noise violators.

NE-3.3 Entertainment Uses. Require noise generating uses, such as restaurants, bars, entertainment venues, and industrial manufacturing operations to minimize noise impacts on adjacent noise-sensitive receptors when there is a potential for adverse noise impacts to occur.

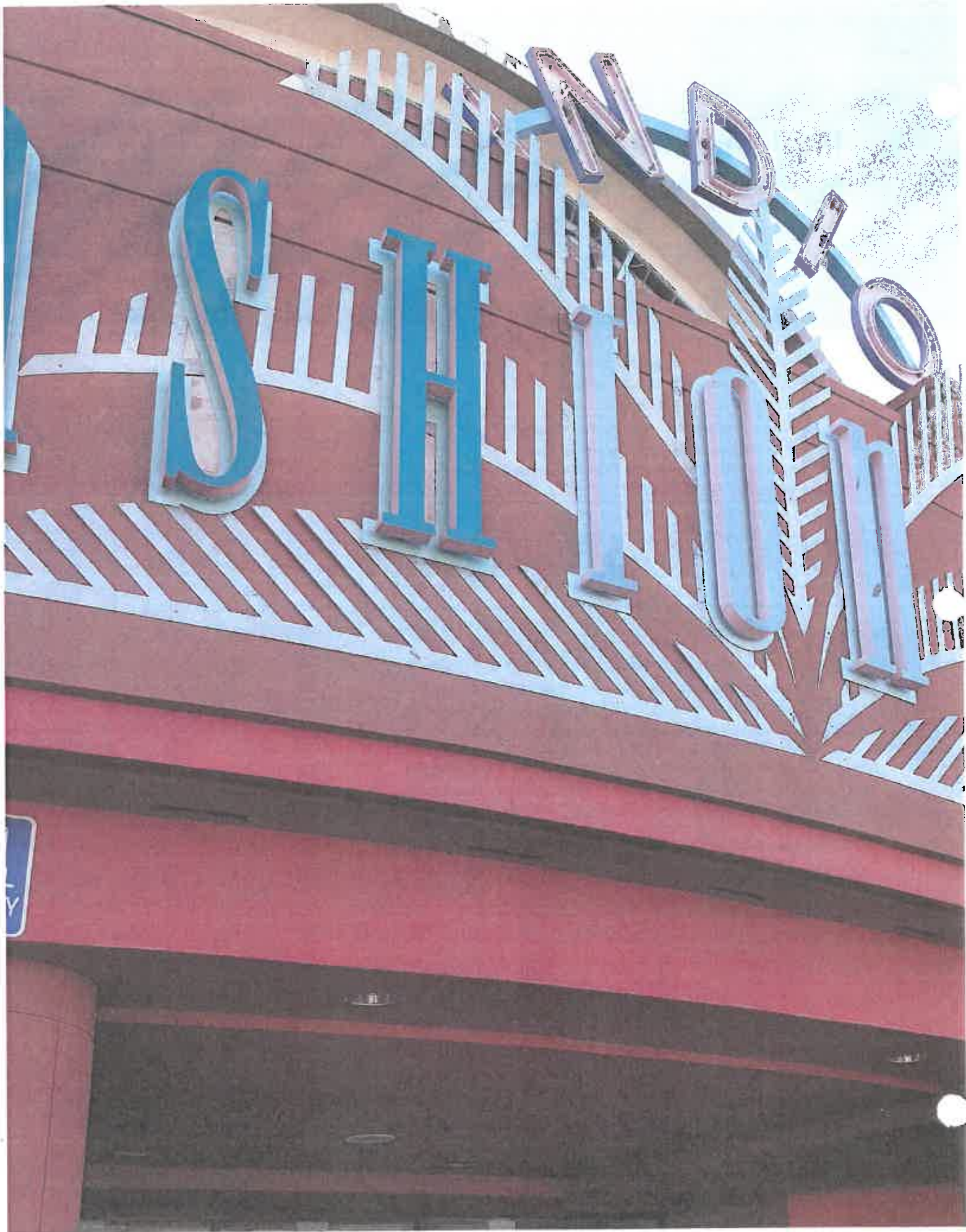
NE-3.4 Construction Noise. Require development to minimize the exposure of neighboring properties to excessive noise levels from construction-related activity during all phases of construction.

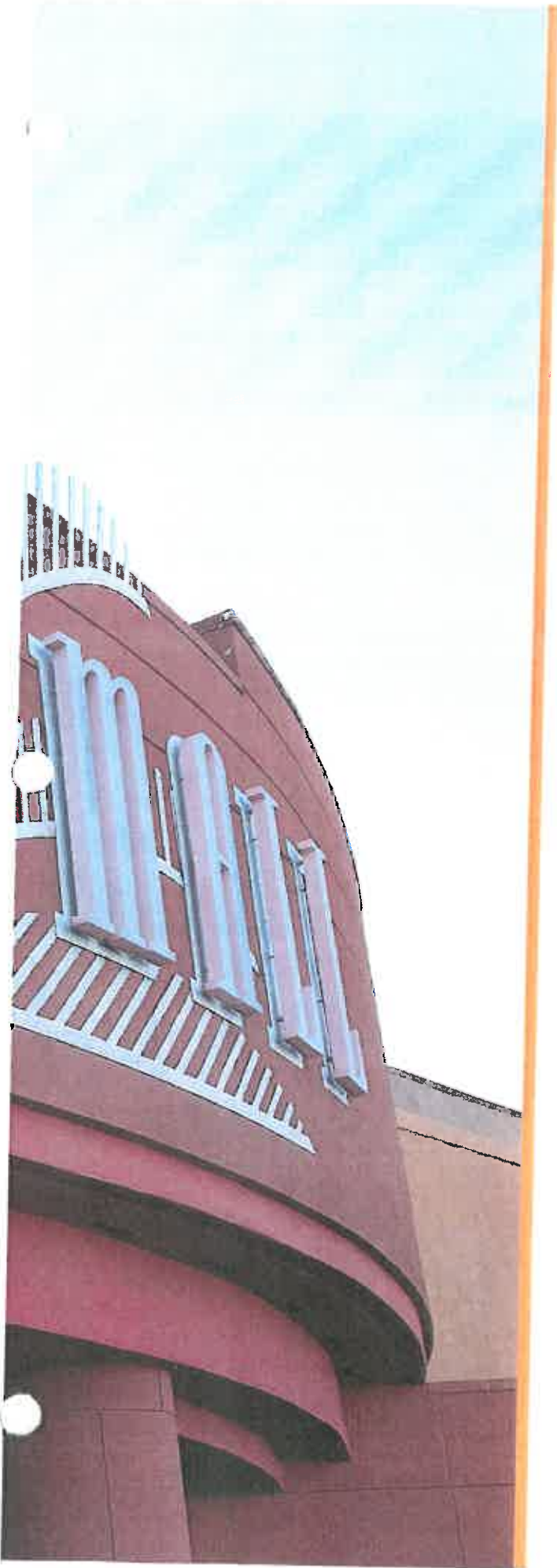
Implementation Actions

The table below identifies programs, policy updates, planning efforts, coordination efforts, and other actions that will help implement the General Plan's noise sources and compatibility. Programs are consistent with this chapter's goals and policies.

Table 11-2: Noise Programs

	DESCRIPTION	PRIORITY	TIME FRAME	RESPONSIBILITY
1	Special Events Noise Mitigation Program. Continue to monitor Special Events noise levels and seeks ways to mitigate for least neighborhood disruption, including in the Major Music Festival Overlay Zone.	High	Ongoing	Planning, Public Works
2	Noise Ordinance. Amend the noise ordinance to include day and nighttime noise level limits and to specifically protect noise sensitive land uses during overnight hours.	High	Short	Planning
3	Airport Overlay. Adopt an overlay zone, as part of the Noise Ordinance or separately, for lands that fall within the airport influence area of the Bermuda Dunes Airport.	Medium	Medium	Planning
4	Construction Noise Limits. Review the hours of allowed construction activity to ensure they effectively lead to compliance within the limits (maximum noise levels, hours and days of allowed activity) established in the City's noise regulations.	Medium	Short	Planning
5	Noise Studies. Require submittal of applicable technical reports prepared by qualified professionals as part of the development review process. Depending on the location, size, or type of development proposed, technical reports could be required, including a noise impact analysis.	High	Ongoing	Planning





chapter twelve

IMPLEMENTATION

This section describes implementation programs for the General Plan. Implementation actions are generally actions needed to direct City Staff and execute specific policies within the General Plan, such as creating an ordinance or updating a master plan. This chapter also includes indicators to track the implementation of the General Plan over time.

Plan Implementation

The General Plan will be implemented over an extended period of time (estimated to be 20 years with a time horizon of 2040). During this time, long-range planning efforts will continue using the goals and policies in the General Plan as a guide.

Although, the General Plan is a living document, State law allows it to be updated and refined over the coming decades. It encourages annual review of implementation actions and recommends that the entire General Plan be thoroughly reviewed every five years to ensure that it is still consistent with the community's goals.

The General Plan may be amended up to four times per year to accommodate changing conditions. Property owners, the Planning Commission, City Council, or City staff may propose amendments. Proposed changes must be reviewed by the Planning Commission and the City Council at public hearings and the potential of environmental impacts must be evaluated in accordance with the California Environmental Quality Act.

This implementation chapter includes programmatic actions to be completed by the City, such as updating plans, completing studies, or implementing new programs. Each implementation program identifies the responsible City Departments, a level of priority (high, medium, or low) and a suggested time frame, as described in Table 12-1.

Table 12-1: Time Frame Guide	
Estimated Date of Completion	
Short	<i>1 – 5 years</i>
Medium	<i>5 – 10 years</i>
Long	<i>10+ years</i>
Ongoing	<i>Recurring or continuous action</i>

Implementation Programs

The tables below identify programs, policy updates, planning efforts, coordination efforts, and other actions that will help implement the General Plan’s vision and policies. Programs are organized by General Plan chapter and are consistent with each chapter’s goals and policies.

Table 12-2: Land Use and Community Design Implementation Programs

	DESCRIPTION	PRIORITY	TIME FRAME	RESPONSIBILITY
1	<p>Specific Plan Consistency. Following adoption of the General Plan, review the existing, adopted Specific Plans and Project Master Plans and make changes to ensure consistency with the General Plan.</p>	High	Short	Planning
2	<p>Update Zoning. Modify zoning standards to promote neighborhood, district, and center design. Zoning standards should consider building intensity, height and massing, placement on a lot, parking location and amount, ground-floor open space, and parking location and access. The revised zoning should include:</p> <ul style="list-style-type: none"> • Form-based regulations and standards that create more certainty in the development review process, and clearly define walkable centers and neighborhoods. Design guidelines to illustrative community character and complement development requirements. • Building design standards to foster growth and development in the existing residential neighborhoods, including by-right up zoning and support for missing middle housing. • Updated subdivision ordinance. • Streamline desired uses to permit uses that foster greater economic opportunity and conform to General Plan by-right, including hospitality, visitor-serving uses, and temporary or pop-up uses. • Allow for reduced parking requirements, unbundled parking, shared parking, and multiple tenant structures that encourage development of shared spaces and services. 	High	Short	Planning
3	<p>Development Review. Work to find efficacies and make development permitting as simple and as transparent as possible.</p> <ul style="list-style-type: none"> • Move elements of permitting process on line where possible • Integrate the permitting and planning with GIS capabilities to aid in program development 	Medium	Ongoing	Planning

	<ul style="list-style-type: none"> • Work to reduce multiple visits per applicant 			
4	Neighborhood Master Planning Process. Review existing project master plan requirements and update the planning process to enable incremental development of vacant parcels while generating interconnected centers, neighborhoods, and employment areas of the City.	High	Short	Planning, Public Works
5	Development Fees: Update citywide development impact fees for infrastructure, affordable housing, other community benefits, and long-range planning.	High	Medium	Planning, Economic Development, Public Works, Parks
6	Business Improvement Districts. Facilitate the formation of Business Improvement Associations (BIA), Business Improvement Districts (BID), or similar to unite business owners into area branding, marketing, on-going maintenance, and beautification efforts. Focus initial efforts in Downtown, Midtown, and Highway 111.	High	Medium	Planning, Economic Development
7	Placemaking Program. Implement a placemaking program focused on cost-effective and flexible activities, such as a “lighter, quicker, cheaper” approach in Midtown and Downtown.	High	Short	Planning, Economic Development
8	Midtown Marketing. Create a Midtown branding and marketing strategy.	Medium	Short	Planning, Economic Development
9	Midtown Master Developer. Study whether to seek a master developer to lead redevelopment efforts in the Midtown subarea.	High	Short	Planning, Economic Development
10	Development Monitoring. Monitor future development and growth in the City and report annual changes to the Planning Commission and City Council. Key metrics may include housing by type and income level, commercial floor area, jobs, vehicle miles traveled, and greenhouse gas emissions.	Medium	Ongoing	Planning
11	Infill Housing Incentive Program. Establish an infill housing incentive program consistent with the Housing Element. Potential incentives may include an expedited building permit process, impact fee waivers, or other incentives.	Medium	Long	Planning, Economic Development
12	General Plan Reviews. Review the General Plan on a five-year cycle, including a review of individual elements and community programs. Periodically revise the General Plan and respond to individual Plan Amendment petitions.	High	Short	Planning, Economic Development

Table 12-3: Mobility Implementation Programs

	DESCRIPTION	PRIORITY	TIME FRAME	RESPONSIBILITY
1	Design Guidelines. Complete the City's complete street design guidelines and update the City's design standards for consistency with complete streets.	High	Short	Public Works
2	Protected Streets and Intersections List. Develop a list of protected streets and intersections that are exempt from a level of service standard (see policy ME-1.8) for any mode or that are exempt from implementing appropriate complete street standards. This list will be maintained by the City's Public Works Department and shall be approved by City Council resolution. Conditions that would warrant a complete streets exception would include inadequate right-of-way, potential impacts to environmentally sensitive areas, the need to modify mode priority due to an adjacent land use compatibility issue, or the need to maintain enhanced designs for other prioritized modes.	High	Short	Public Works
3	Development Review. Update the City's site plan review process and subdivision ordinance to require the provision of adequate bicycle and pedestrian access for new development projects.	High	Short	Planning, Public Works
4	SB 743 Monitoring. Monitor implementation of SB 743 and update the City's CEQA guidelines to reflect revisions in CEQA requirements. The City's consistency with SB 743 requirements does not relieve development from maintaining the City's level of service requirements noted in Policy ME-1.8.	Medium	Short	Public Works
5	Truck Routes. Update the designated truck routes shown on Figure 4-8 to allow the safe and efficient movement of goods for commerce and industry, minimize conflicts with preferred modes (e.g. minimize truck routes along bicycle or pedestrian prioritized corridors), and minimize incompatibility with other sensitive land uses in the City including residential neighborhoods. The updated truck route map will be provided on the City's website and will be adopted by City Resolution.	High	Short	Public Works
6	Curb-Space Allocation. Develop curb space priorities and guidelines to assist with actively managing increased demands on curb space utilization.	Medium	Medium	Public Works
7	Parking Requirements. Revise the Zoning Ordinance requirements for off-street parking to ensure consistency with the General Plan.	High	Short	Public Works, Planning

8	Dedicate City Staff for Complete Streets and Safe Routes to School Projects. Allocate additional staff resources to manage, implement and pursue funding for complete street projects, including the submittal of grant applications for bikeway, pedestrian and Safe Routes to School (SRTS) funding.	High	Ongoing	Public Works
9	Vehicle Idling. Establish a local ordinance that exceeds the state vehicle idling restrictions where appropriate, including restrictions for bus layovers, delivery vehicles, trucks at warehouses and distribution facilities and taxis, particularly when these activities take place close to sensitive land uses (schools, senior centers, medical facilities and residences).	Medium	Medium	Public Works, Planning
10	Collision Analysis. Analyze areas of high risk of pedestrian or cyclist danger and develop a priority list of future physical improvements that consider future land uses within the area.	High	Short	Public Works, Planning

Table 12-4: Economic Development Implementation Programs

	DESCRIPTION	PRIORITY	TIME FRAME	RESPONSIBILITY
1	<p>Update Economic Development Planning. Continue to update and implement an economic development strategy. The strategy should be based on analysis, community and stakeholder engagement, General Plan goals and policies, and City Council priorities. In addition to the vision, goals, objectives, and actions generated during preparation of the strategy, it should actively seek to achieve the goals of the General Plan, and it may provide or address the following:</p> <ul style="list-style-type: none"> • A business visitation program • A communications strategy to market Indio to existing businesses, residents, and potential new business • Specific business types to target for business attraction efforts • An incentives policy • Measurable objectives for local economic development efforts and for collaboration with regional economic development partners 	High	Ongoing	Economic Development, community partners

	<ul style="list-style-type: none"> • Economic development performance benchmarks and targets • A mechanism to measure performance and adjust programs where needed to improve performance. 			
2	<p>Establish and implement an annual business visitation program. Focus on attracting and recruiting new investment in key sectors with a focus on hospitality and retail.</p> <ul style="list-style-type: none"> • Participate in investment events such as ICSC conferences • Commission a hospitality market study and identify sites for future investment • Maintain and update inventory of sites for commercial development 	High	Ongoing	Economic Development, community partners
3	<p>Establish, update, and publish an inventory of sites and facilities. Continue to make inventory of available spaces for economic development and new investments available. Keep information up to date and available on line.</p> <ul style="list-style-type: none"> • Provide information of commercial development sites by type of land use • Provide information on large scale residential developments 	High	Ongoing	Economic Development, community partners
4	<ul style="list-style-type: none"> • Downtown Indio Specific Plan. Market the Downtown Specific Plan to developers and businesses generally conducive to a pedestrian-oriented Downtown area, such as a mix of local and regional-serving retail, local serving office uses, arts and cultural facilities, and cafes / restaurants. 	High	Ongoing	Economic Development, Planning
5	<p>Midtown District. Market the Midtown District to developers and businesses to revitalize the area around the Fashion Fair Mall.</p>	High	Short	Economic Development, Planning
6	<p>Streamline Development Permitting Process. Work to find efficacies and make development permitting as simple and as transparent as possible.</p> <ul style="list-style-type: none"> • Move elements of permitting process on line where possible • Integrate the permitting and planning with GIS capabilities to aid in program development <p>Work to reduce multiple visits per applicant</p>	Medium	Ongoing	Planning
7	<p>Marketing and Placemaking Programs. Develop marketing and beautification strategies to raise awareness of areas targeted for revitalization.</p>	High	Short	Economic Development, Planning

8	<p>Workforce Training Inventory. Conduct an inventory of workforce training needs of businesses in Indio and coordinate with regional partners and other educational institutions to identify or create suitable training programs and help coordinate training resources in the County.</p>	High	Short	Economic Development
9	<p>Public-private partnerships. Support the use of public-private partnerships to foster job growth and vocational training, including partnerships used or planned by major public entities in Indio.</p>	Medium	Ongoing	Economic Development

Table 12-5: Health and Equity Implementation Programs

	DESCRIPTION	PRIORITY	TIME FRAME	RESPONSIBILITY
1	Health Resources Inventory. Develop an inventory of health resources in the City in cooperation with the Riverside County University Health System and local hospital system.	Medium	Short	Planning
2	Healthy Food Plan. Develop a plan to attract more healthy food choices for residents of the City. The plan should strategically consider location and expansion of new and existing businesses to promote access to healthy food choices, including farmers' markets (see action 3), throughout the City, including incentives to locate or expand grocery stores and other healthy food purveyors throughout the City.	Medium	Long	Planning
3	Farmers' Market. Pursue funding and partner with local organizations to provide a year-round farmers market in the City and seek out additional vendors to participate in the market.	Medium	Medium	Planning
4	Health Fitness Challenges. Create and implement health fitness challenges for City employees, businesses, and residents to encourage healthy living.	Low	Medium	Community Services
5	Health Fair. Host an annual Health Day Fair with information, health care screenings and services, and activities celebrating an active lifestyle. The event should be sponsored by the City and involve a wide range of service providers.	Medium	Ongoing	Community Services
6	Employee Health and Wellness Program. Implement employee health and wellness programs such as: <ul style="list-style-type: none"> • Offering discounts for gym or health club memberships; • Offering onsite health clinics for employees; • Bringing in exercise instructors to teach lunchtime classes; • Distributing educational information about physical fitness, nutrition, and other health topics to employees; • Organizing competitive events that encourage employees to be physically active; and • Prohibiting smoking and vaping on City property. 	Medium	Medium	Community Services
7	Town Hall Meetings. Hold bi-annual town hall meetings to implement the General Plan. Publicize the meetings widely and in multiple languages.	Medium	Short	Planning

8	<p>Zoning Code Updates. Amend City codes and ordinances for their impact on health. Topics that should be addressed include:</p> <ul style="list-style-type: none"> • Location of fast food restaurants and liquor stores. • Standards for improved indoor air quality resulting from pollution such as toxics, VOCs, mold, paint, asbestos, smoking, and other chemicals. • Encouragement for HVAC systems that achieve high levels of indoor air quality. • Reduction of dust and other emissions from construction sites and unpaved industrial areas of the City. • Standards for multi-family buildings that locate along freeways or truck routes or near hazardous materials. 	High	Short	Planning
9	<p>Development Checklist. Coordinate with the Riverside County Public Health Department to implement a land development review checklist to ensure that projects enhance public health outcomes. The checklist should address topics such:</p> <ul style="list-style-type: none"> • Pedestrian environment • Building siting • Access to transit • Access to parks • Proximity to healthy food sources • Proximity to existing or potential sources of pollution (such as freeways and land uses that use hazardous materials) • Shade and tree canopy 	High	Short	Planning
10	<p>Smoke-Free Events. Support smoke-free public events and facilities, including farmers’ markets, public parks and trails, and community street fairs.</p>	Medium	Ongoing	Community Services
11	<p>Regional Coordination. Cooperate with local, regional, and state agencies that work to improve air quality and mitigate negative health impacts.</p>	Medium	Ongoing	Planning
12	<p>Vandalism and Maintenance Reporting. Create a program and/or method for residents to monitor and report vandalism along with maintenance issues in parks and public Right-of-Way.</p>	Medium	Medium	Public Works
13	<p>CPTED Manual. Develop a development manual to provide basic requirements and incentives for the inclusion of design features in new development to</p>	Medium	Medium	Planning

	reduce potential for crime. These features could include well-lighted parking areas, open landscaping, limited access into and between buildings, and limited access to rooftops.			
14	Learning Partnerships. Partner with local institutions and other public and private organizations to maintain and enhance existing extended learning opportunities through after-school enrichment programs, summer boost programs, school breaks, and weekends for all children.	High	Medium	Community Services
15	Housing Element. Incorporate analysis of key livability issues, including overcrowding, lead exposure, and other substandard housing conditions, in the next housing element update.	High	Short	Planning

Table 12-6: Parks, Recreation, and Open Space Implementation Programs

	DESCRIPTION	PRIORITY	TIME FRAME	RESPONSIBILITY
1	<p>Park Master Plan. Prepare a Park Master Plan that includes the following:</p> <ul style="list-style-type: none"> • A comprehensive inventory of the City’s existing and planned parks and recreational facilities, including regional park and open space initiatives. • A comprehensive inventory of the City’s existing and planned trails. • An assessment of future park and recreational needs. • Preferred planning and zoning tools for new development, such as land dedication and in lieu fees. • Park funding and financing options, including mechanisms to maintain existing and future park resources, including development extract, general fund, bonds, and grants. 	High	Short	Planning, Community Services
2	<p>Park and Recreational Facility Fee Study. Conduct a park and recreational facility fee study, in conjunction with other fee studies defined in the General Plan, to support updated park and recreational impact fees.</p>	High	Short	Planning, Community Services
3	<p>Urban Agriculture. Amend the Municipal Code to implement the General Plan Update to support urban agriculture. Adopt regulations that specify the requirements for community gardens and other urban agricultural uses, such as farm stands, farmers’ markets, and animal keeping.</p>	Medium	Short	Planning
4	<p>Community Garden Program. Establish a Community Garden Program in the City.</p>	Medium	Medium	Community Services, Planning
5	<p>Park Placemaking. Create incentives for converting vacant, underutilized, or parking lots into small parks or open spaces throughout the City.</p>	Medium	Short	Planning
6	<p>Increase Parkland. Establish a funding source to increase parkland within Indio by transforming abandoned utility corridors into linear parks, reclaiming unused street rights-of-way, or purchasing fallow farmland.</p>	Medium	Short	Planning and Public Works
7	<p>Trail Signage. Develop trail signage program to include signage along trails for interpretative</p>	Low	Short	Public Works

	purposes, and to display rules of appropriate use and conduct on trails.			
8	Edible Landscaping Program. Develop a program that permits planting fruit trees within parks or on other City property. Coordinate with non-profit organizations or community volunteers to identify funding sources, plant and maintain the trees, and harvest the fruit.	Low	Short	Planning and Public works, Parks
9	Joint Use Agreements. Create joint-use agreements with Desert Sands Unified School District to maximize shared community use of school facilities and City park facilities, where appropriate, to expand opportunities for physical activity.	High	Short	Planning, Community Services

Table 12-7: Conservation Implementation Programs

	DESCRIPTION	PRIORITY	TIME FRAME	RESPONSIBILITY
1	<p>Staff Allocation. Designate a City staff member(s) responsible for overseeing implementation of its energy and water conservation programs. Example activities may include:</p> <ul style="list-style-type: none"> • Manage the residential energy disclosure program. • Promote installation of solar panels and implementation of energy upgrades through Indio’s Ygrene and HERO Programs. • Benchmark municipal facilities using the Environmental Protection Agency’s ENERGY STAR Portfolio Manager or equivalent benchmarking tool and identify energy efficiency upgrades in City buildings. • Establish an employee carpooling program. • Provide lectures, seminars, and training on green building based on guide and training material emphasizing desert conditions and opportunities. 	High	Short	Sustainability Coordinator
2	<p>Residential Transfer of Title Energy Disclosures. Establish an energy disclosure program to be performed prior to sale or other transfer of title. Disclosures should provide prospective owners with recommendations for retrofit measures to be given to the buyer prior to transfer of title.</p>	High	Short	Planning, Sustainability Coordinator
3	<p>Low Income Weatherization Assistance Program. Partner with Imperial Irrigation District and SoCal Gas to aggressively promote existing programs that provide financial assistance to low-income households for weatherization improvements and heating, ventilation, and air conditioning (HVAC) tune-ups.</p>	High	Ongoing	Sustainability Coordinator
4	<p>Solar Financing. Formalize and implement a protocol to promote installation of solar panels by continuing to support Indio’s Ygrene and HERO Program and by distributing information on actual savings achieved by PV systems.</p>	High	Short	Sustainability Coordinator
5	<p>Landscape and Water Conservation Ordinance. Adopt the Coachella Valley Model Water Efficient Landscape Ordinance.</p>	High	Short	Planning, Public Works

6	<p>Greywater Ordinance Study and adopt an ordinance allowing greywater systems that redirect water from washbasins, showers, and tubs.</p>	Medium	Medium	Planning, Public Works
7	<p>Zoning and implementation ordinances. Regularly review and update zoning and building codes to enable innovative sustainability measures, including implementation of the following:</p> <ul style="list-style-type: none"> • Permit approval streaming for renewable energy or energy upgrades • Greywater capture and reuse systems • Wind generation on residential and commercial buildings • Electric vehicle infrastructure requirements • Green building performance standards, such as those to achieve Net Zero goals • Dark Sky Ordinance 	High	Ongoing	Planning, Public Works, Sustainability Coordinator
8	<p>Heritage Trees. Identify heritage trees that should be preserved. Trees on this list shall only be removed when a public interest served by removal outweighs the interest in preservation and heritage status.</p>	Medium	Ongoing	Community Services, Planning
9	<p>Agricultural Study. Develop a study of sound agricultural practices that addresses surface and groundwater contamination, particulate emissions from agricultural operations, minimal soil erosion, and the buildup of salts in soils. Based on the study, create brochures available to the agricultural community.</p>	Low	Short	Community Services
10	<p>Historic Resources Program. Establish a program and identify potential funding sources to facilitate the preservation and maintenance of identified historic resources. Ensure that any proposed changes to such landmarks are appropriate to the historic character of the original structure.</p>	Medium	Short	Planning
11	<p>Historic Resources Design Guidelines. Prepare and adopt design guidelines that contribute to the creation of a distinctive character for each subarea based on important historic/cultural resources within each subarea.</p>	Medium	Short	Planning

Table 12-8: Infrastructure and Public Facilities Implementation Programs

	DESCRIPTION	PRIORITY	TIME FRAME	RESPONSIBILITY
1	Plan Consistency. Following adoption of the General Plan, review the existing utility master plans and make changes to ensure consistency with the General Plan.	High	Short	Public Works
2	City Ordinances. Update the existing City code and ordinances to reflect current requirements and GP policies for water, sewer, and storm drainage requirements and policies. This should include low-impact development practices which improve water quality and promote groundwater recharge, increase on-site infiltration.	High	Short	Public Works, Planning
3	Groundwater Management Plan. Complete the Groundwater Management Plan, as required by State law. Oversee the completion of the Plan and provide materials and resources as needed.	Medium	Short	Public Works
4	Solid Waste Diversion. Establish a waste diversion plan with in a minimum diversion rate of 75% of solid waste generated from landfills.	High	Short	Public Works
5	Multifamily Recycling. Establish a protocol to expand outreach and technical assistance to multifamily buildings to increase recycling in accordance with AB341.	Medium	Short	Public Works
6	Green Purchasing. Implement a “green purchasing” program that minimize impacts to the environment from production to disposal. The program should address product stewardship and producer responsibility.	Medium	Short	City Manager
7	Food share programs. In conjunction with community partners and neighborhood organizations, explore and pilot food share programs reduce food waste.	Low	Medium	Planning, Public Works.
8	Event waste diversion. Require recycling and food waste diversion at special events, including City-hosted events.	High	Short	Public Works
9	Neighborhood Clean-up Program. In coordination with the Housing and Development Services Department, host community and neighborhood clean-up program, tree planting, and other beautification programs.	Low	Short	Planning, Public Works.
10	Development impact fees. Adopt nexus-based impact fees that mitigate the cost of providing infrastructure and public facilities to serve new development. Address infrastructure improvements that are	High	Ongoing	Planning, Public Works

	necessitated by new development – including water, sewer, stormwater, and utilities. Seek additional funding mechanisms as necessary.			
11	Sewer and Stormwater user fees. Update existing user fees for sewer and stormwater to fund needed system upgrades and, to the extent feasible, allow for waste water recycling and stormwater capture for beneficial use.	Medium	Medium	Planning

Table 12-9: Safety Implementation Programs

	DESCRIPTION	PRIORITY	TIME FRAME	RESPONSIBILITY
1	Plan Consistency. Following adoption of the General Plan, review the existing hazard mitigation and emergency operations plans and make changes to ensure consistency with the General Plan.	High	Ongoing	Emergency Services Coordinator
2	California Building Codes. As new versions of the California Building Standards Code (CCR Title 24, published triennially) are released, adopt and enforce the most recent codes. Specifically, to minimize damage from earthquakes and other geologic activity, implement the most recent State and seismic requirements for structural design of new development and redevelopment.	Medium	Ongoing	Planning, Public Works
3	Develop City Standards and Guidelines. The City shall develop standards and guidelines and require their use in new construction to provide the greatest possible protection for human life and property in areas where there is a high risk of seismic or geologic hazard occurrence. This could include requirements for geotechnical reports and seismic safety plans and setbacks for new construction, among others.	Medium	Short	Planning
4	Maintain GIS Database. Maintain the City's Geographic Information System (GIS) database to assist hazard planning, including: <ul style="list-style-type: none"> ◦ Critical facilities • Vulnerable populations • Infrastructure 	High	Ongoing	Planning, Public Works
5	Critical Facility Insurance Coverage. Document current City insurance coverage on critical City facilities. Collect all costs associated with mitigation from a disaster and operating costs as required by Federal funding eligibility.	High	Short	Emergency Services Coordinator
5	Residential Disaster Response Plans. Establish a program that requires all residential associations over 100 homes to establish a disaster response plan and practice the plan regularly. Establish a disaster response drill/exercise schedule requirement and practice annually.	Low	Ongoing	Emergency Services Coordinator, Planning
6	Emergency Management. Establish an ordinance for anti-price gouging, on call debris management contractors, and pre-identified mass care shelter locations.	Low	Short	Emergency Services Coordinator

7	CERT Newsletter. Regularly publish a CERT newsletter.	Low	Short	Emergency Services Coordinator
8	Reduce Fire Hazards. Amend the Municipal Code to include weed abatement/brush management regulations to reduce fire hazards and ensure adequate coordination between planning and fire issues.	Medium	Short	Planning, Emergency Services Coordinator
9	Hazard Impact Assessment. Hire a consultant to conduct an Hazards Impact Assessment for City of Indio that will identify losses to revenue, and recovery time objectives for businesses and the City.	Medium	Medium	Emergency Services Coordinator
10	Disaster Mitigation Funding. Consider applying for Mitigation grant funding opportunities before a disaster (Stafford Act Section 404) and prepare as much as possible for mitigation projects after a disaster (Stafford Act Section 406). Typically private property is not eligible, but there may be other funding opportunities for HOAs, etc.	Medium	Ongoing	Emergency Services Coordinator
11	Hazardous Mitigation Training Course. Implement an 8-hour hazardous mitigation basic training course requirement for new hires of the City of Indio.	Low	Short	Emergency Services Coordinator
12	Climate Vulnerability Assessment. Complete a climate vulnerability assessment consistent with the requirements of SB 379 at the time of the next Housing Element update.	High	Short	Planning, Emergency Services Coordinator

Table 12-10: Noise Implementation Programs

	DESCRIPTION	PRIORITY	TIME FRAME	RESPONSIBILITY
1	Special Events Noise Mitigation Program. Continue to monitor Special Events noise levels and seeks ways to mitigate for least neighborhood disruption, including in the Major Music Festival Overlay Zone.	High	Ongoing	Planning, Public Works
2	Noise Ordinance. Amend the noise ordinance to include day and nighttime noise level limits and to specifically protect noise sensitive land uses during overnight hours.	High	Short	Planning
3	Airport Overlay. Adopt an overlay zone, as part of the Noise Ordinance or separately, for lands that fall within the airport influence area of the Bermuda Dunes Airport.	Medium	Medium	Planning
4	Construction Noise Limits. Review the hours of allowed construction activity to ensure they effectively lead to compliance within the limits	Medium	Short	Planning

	(maximum noise levels, hours and days of allowed activity) established in the City's noise regulations.			
5	Noise Studies. Require submittal of applicable technical reports prepared by qualified professionals as part of the development review process. Depending on the location, size, or type of development proposed, technical reports could be required, including a noise impact analysis.	High	Ongoing	Planning

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.1

HEARING DATE: October 11, 2018

CASE NUMBER: ZAP1069PS18 – J.J.L. Ibarra, (Representative: Wendell Veith, Architect)

APPROVING JURISDICTION: City of Cathedral City

JURISDICTION CASE NO: CUP18-009 (Conditional Use Permit)

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends that the Commission find the Conditional Use Permit CONSISTENT, subject to the conditions included herein.

PROJECT DESCRIPTION: The applicant proposes a 2,602 square foot automobile repair shop building on a 0.2-acre parcel.

PROJECT LOCATION: The site is located on the northwest corner of Sarah Street and Agua Caliente Trail, within the City of Cathedral City, approximately 4,140 feet easterly of the southerly end of Runway 13L-31R at Palm Springs International Airport.

LAND USE PLAN: 2005 Palm Springs International Airport Land Use Compatibility Plan

- a. Airport Influence Area: Palm Springs International Airport
- b. Land Use Policy: Compatibility Zones C, D
- c. Noise Levels: Below 60 CNEL from aircraft noise

BACKGROUND:

Non-Residential Average Intensity: Pursuant to the Airport Land Use Compatibility Plan for Palm Springs International Airport, the site is located within Compatibility Zones C and D (Zone C 0.17 acres, Zone D 0.04 acres). Non-residential intensity in Compatibility Zone C is restricted to an average intensity of 75 people per acre, and Compatibility Zone D is restricted to 100 people per acre. (The proposed building is located entirely within the Zone C portion of the property.)

The total site consists of 0.2 acres, with a maximum allowable intensity of 15 persons. However, the portion of the site that is located within Compatibility Zone C is 0.17 acres, which allows for an intensity of 13 persons. Including the street half-widths for Agua Caliente Trail and Sarah Street (0.06 acres for Sarah Street and 0.08 acres for Agua Caliente Trail), the gross area of the site located within Zone C increases to 0.31 acres, which would allow a maximum allowable intensity of 23 people.

The “Building Code Method” for calculating intensity utilizes “minimum floor area per occupant” criteria from the Building Code as a factor in projecting intensity. Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, the following intensities were utilized for the project:

- office area – 1 person per 200 square feet,
- storage area – 1 person per 300 square feet,
- repair area – 1.5 person per vehicle repair bay.

The proposed building includes 4 service bays where vehicles will be serviced by employees and not opened to the public, 144 square feet of office area, 275 square feet of first floor storage area, and 682 square feet of second floor mezzanine storage area. These areas may be expected to accommodate 10 people, resulting in a prorated average intensity of 32 people per gross acre (including the street half-widths), which is consistent with the Compatibility Zone C average acre criterion of 75.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per standard vehicle). Based on the number of required parking spaces (11 spaces), the total occupancy would be estimated at 17 people. This total occupancy results in an average intensity of 55 people per gross acre (including the street half-widths), which is also consistent with the Compatibility Zone C average acre criterion of 75.

Non-Residential Single-Acre Intensity: As previously noted, the proposed building is located within Compatibility Zone C, which restricts non-residential intensity to 150 people in any given single acre. However, as the site is less than 1 acre in area, this level of occupancy would not be consistent at this site, so single-acre intensity is not applicable in this case.

Prohibited and Discouraged Uses: The applicant does not propose any new use specifically prohibited or discouraged in Compatibility Zones C and D of the Palm Springs International Airport Influence Area.

Noise: The Palm Springs Airport Land Use Compatibility Plan depicts the site as being in an area below 60 CNEL from aircraft. Therefore, special measures to mitigate aircraft-generated noise would not be required.

Part 77: The elevation of Runway 13L-31R at its southerly terminus is 404.4 feet above mean sea level (AMSL). At a distance of approximately 4,140 feet from the runway to the project, Federal Aviation Administration (FAA) review would be required for any structures with a top point elevation exceeding 445.8 feet AMSL. The site's elevation is 369.4 feet AMSL and the proposed structure height is 24 feet, for a top point elevation of 393.4 feet AMSL (lower than the runway elevation). Therefore, review by the FAA Obstruction Evaluation Service for height/elevation reasons is not required.

Open Area: Compatibility Zone C requires 20% and Zone D requires 10% of the land area within major projects (10 acres or larger) be set aside as open area that could potentially serve as emergency landing areas. The proposed project is 0.2 acres in area, so open area is not required.

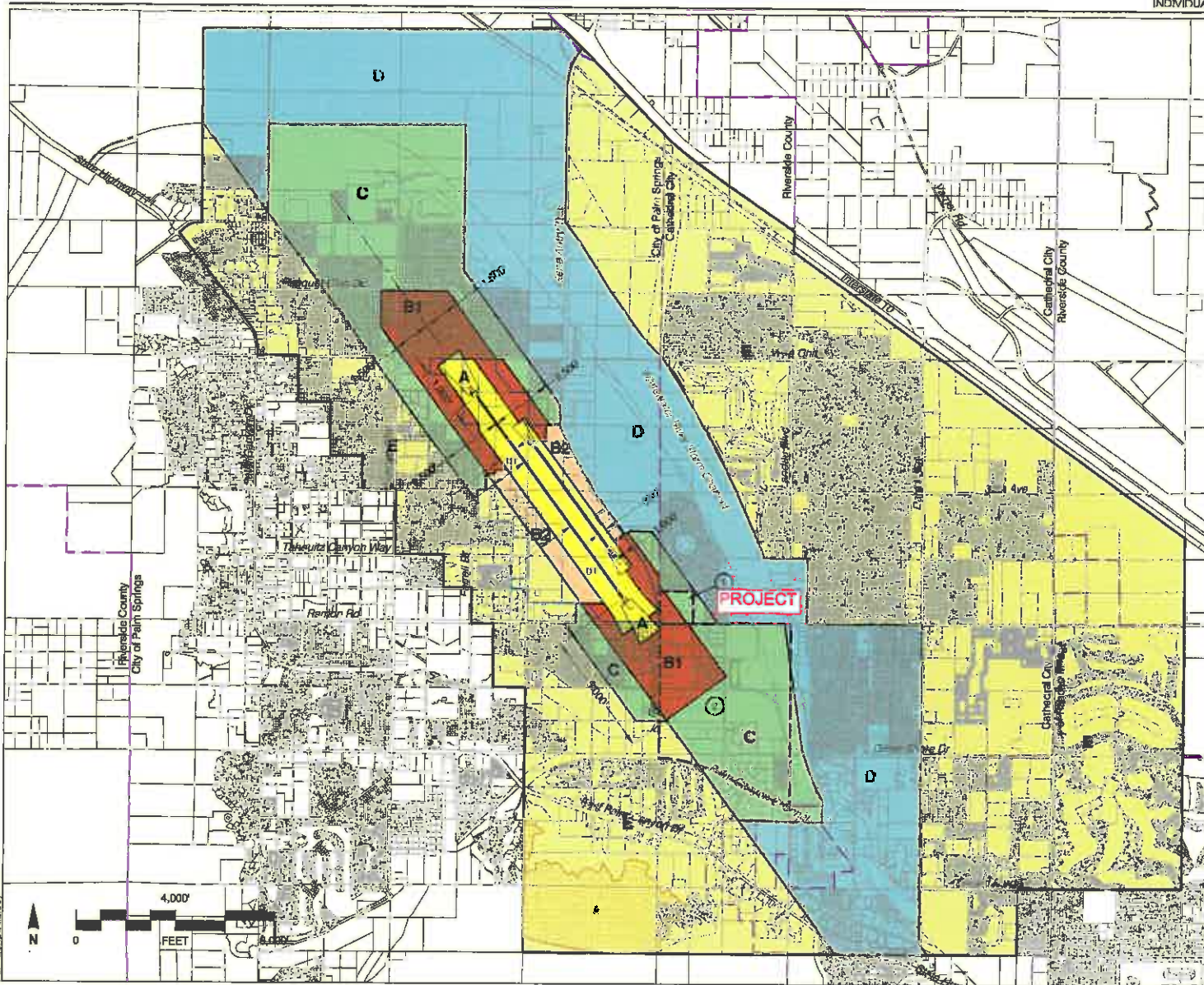
CONDITIONS:

1. Any new outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.

- (e) Children's schools, day care centers, libraries, hospitals, nursing homes, and highly noise-sensitive outdoor nonresidential uses.
- 3. The attached notice shall be given to all prospective purchasers and/or tenants of the property, and shall be recorded as a deed notice.
- 4. Any new detention basin(s) on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping. Trees around the basin(s) shall not form a contiguous canopy and shall not produce fruit, seeds, or berries.
- 5. The evaluated project consists of a 2,602 square foot building for repair and servicing of motor vehicles. Any proposal to use the building for retail or assembly occupancies will require an amended review by the Airport Land Use Commission.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Legend

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E
- Height Review Overlay Zone

Boundary Lines

- Airport Property Line
- City Limits

Notes

All dimensions measured from runway ends and centerlines.

DT = Displaced Threshold

See Chapter 2, Table 2A for compatibility criteria associated with this map.

See Policy PS.2.1.

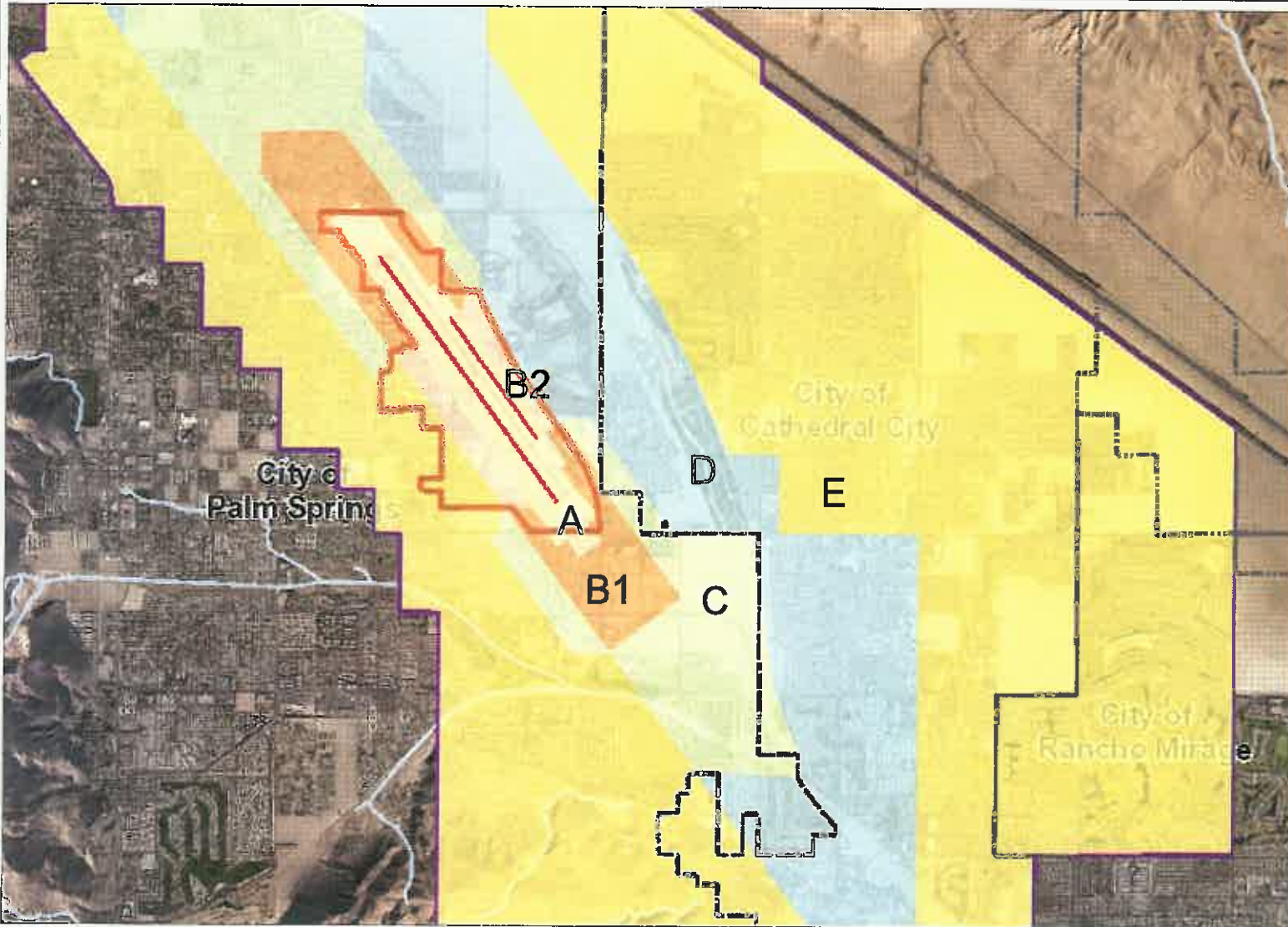
Riverside County
 Airport Land Use Commission
**Riverside County
 Airport Land Use Compatibility Plan
 Policy Document**
 (Adopted March 2005)

Map PS-1

Compatibility Map
 Palm Springs International Airport

FSP-compatibility

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- ▨ OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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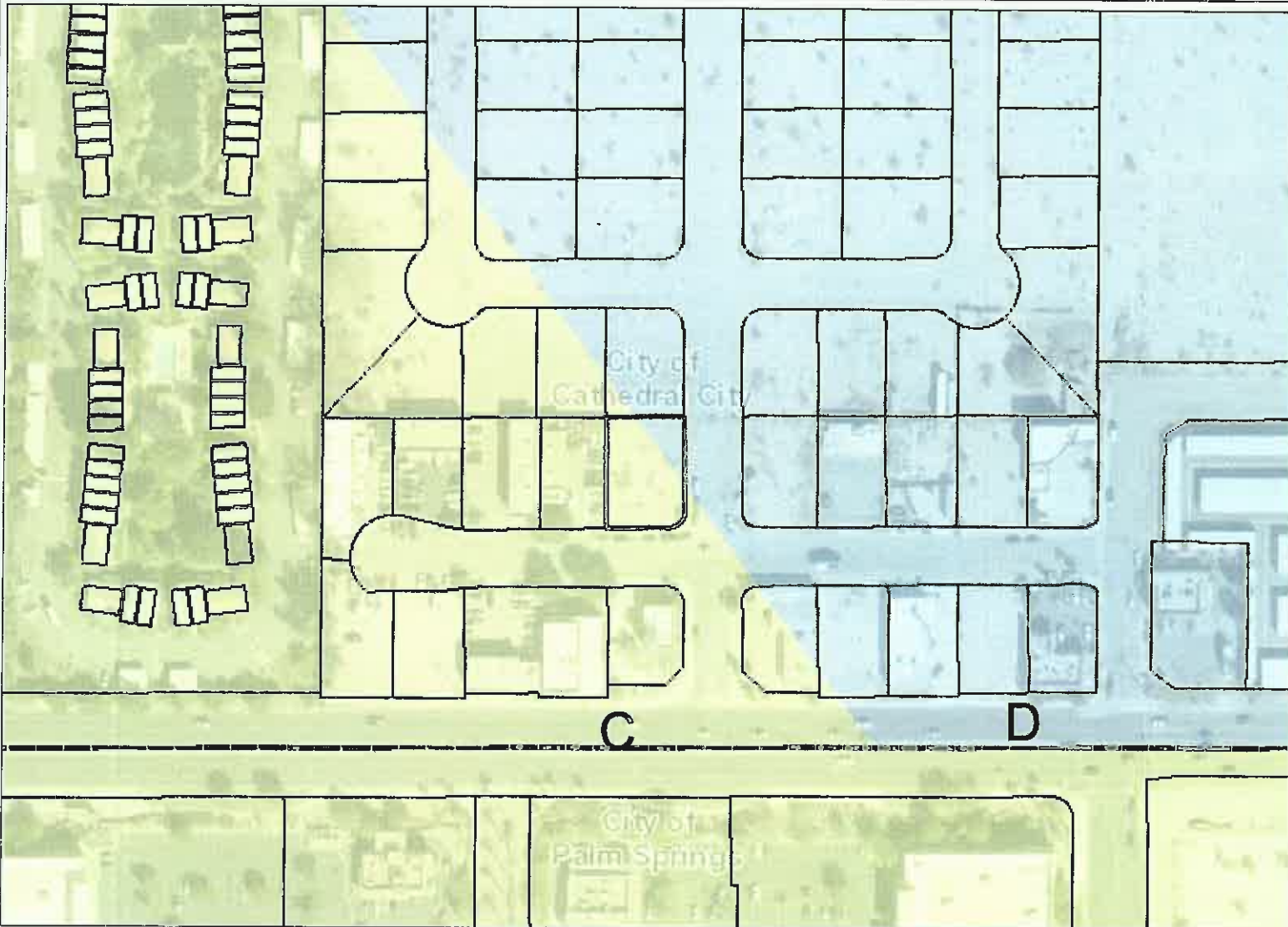


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Notes

Map My County Map





Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas

Airport Compatibility Zones

- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5

0 192 385 Feet

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Notes

Map My County Map



Legend

-  Blueline Streams
-  City Areas
-  World Street Map

Notes



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Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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Notes

Map My County Map



Legend

- Parcels
- Blueline Streams
- City Areas
- World Street Map

Notes



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Map My County Map



Legend

-  Parcels
-  Blueline Streams
-  City Areas
-  World Street Map



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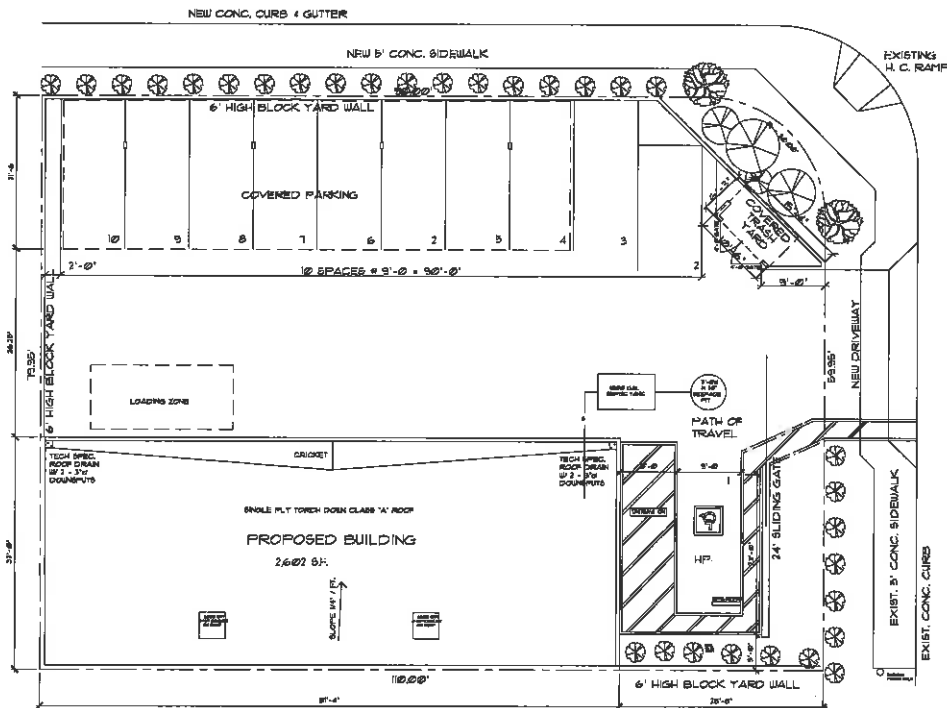
0 192 385 Feet

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Notes

AGUA CALIENTE TRAIL



SITE & ROOF PLAN SCALE: 1/8" = 1'-0"

LEGAL DESCRIPTION:
 LOT 202, PALM SPRINGS OUTPOST UNIT NO. 5
 SECTION 17, T49 R5E S. 2. B. 5. & PL
 A. P. N. 671-446-022

SEE CIVIL ENGINEERING GRADING PLAN FOR DRAINAGE PLAN

SARAH STREET

LANDSCAPE PLANT LEGEND:



WASHINGTONIA ROBUSTA
 MEXICAN FAN PALM
 15' HIGH



FREYCANTHEA ROSEAE
 FREYCANTHEA ESPALIER
 5 GALLON



LEICOPHYLLUM FRUTESCENS
 TEXAS RANGER SHRUBS
 1 GALLON & 5 GALLON

BETWEEN PLANTS PLACE
 SOUTHWEST BLONDE CRUSHED ROCK

PROVIDE DRIP SYSTEM FOR EACH PLANT
 TIE INTO WATER MAIN IN FRONT OF BLDG.
 SHUTOFF VALVE.

SITE DATA:

SITE AREA: 8,794.5 SF.
 BUILDING AREA: 2,662 SF.
 COVERED PARKING / TRASH YARD: 1,152 SF.
 II PARKING SPACES REQUIRED
 10 REG. PARKING SPACES + 1 H.C. SPACE PROVIDED

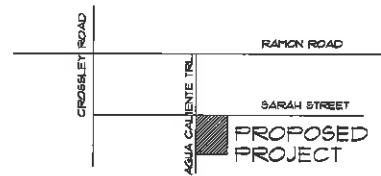
APPLICANT:

J. JESUS LOERA & MARIA RODRIGUEZ
 APN: 871-446-022-4
 LOT 202, PALM SPRINGS OUTPOST UNIT 5
 S 1/2, SW 1/4, SECTION 17, T49, R5E, MB 26/05

UTILITY PROVIDERS

SOUTHERN CALIFORNIA EDISON
 SOUTHERN CALIFORNIA GAS CO.
 DESERT WATER AGENCY
 BURRTEC WASTE DISPOSAL

VICINITY MAP:



NOT TO SCALE



ARCHITECT



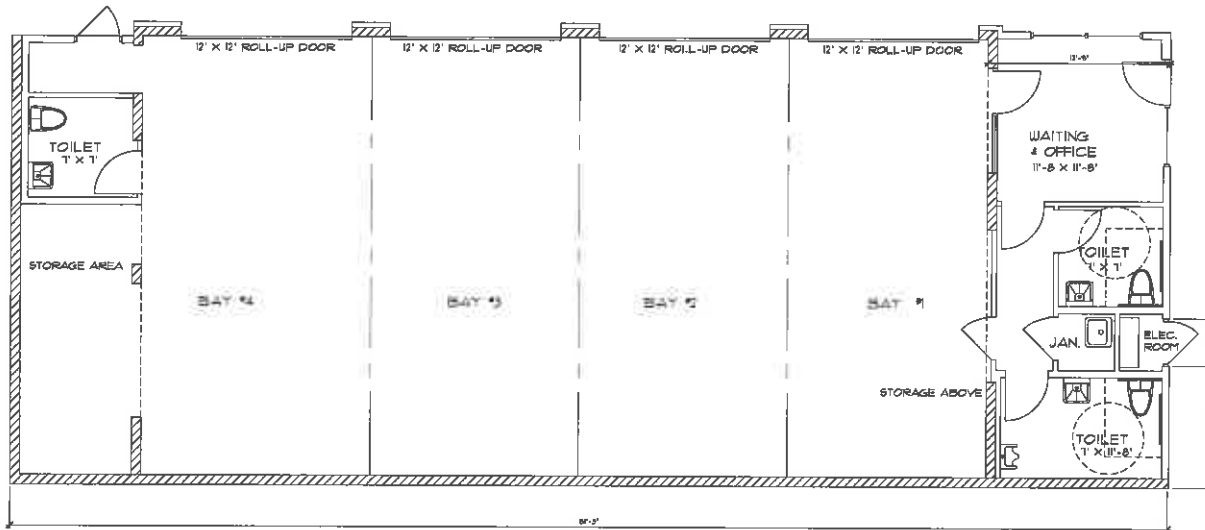
W. VEITH

PROPOSED AUTO REPAIR FACILITY
CHUEY'S AUTO REPAIR
 SARAH STREET, CATHEDRAL CITY, CA.

PRELIMINARY SITE PLAN

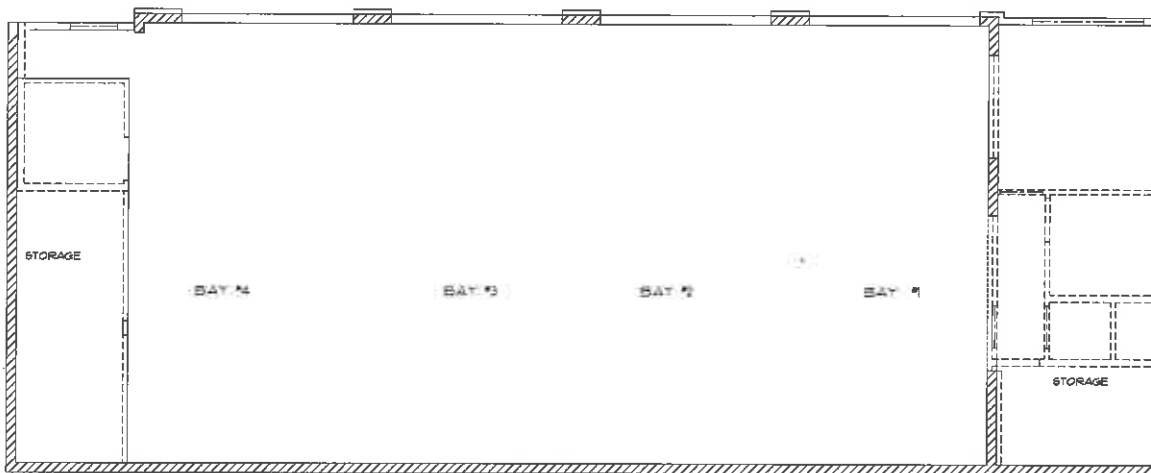
DATE:

51



FLOOR PLAN

3481 SQ. FT.



STORAGE AREA PLAN



ARCHITECT



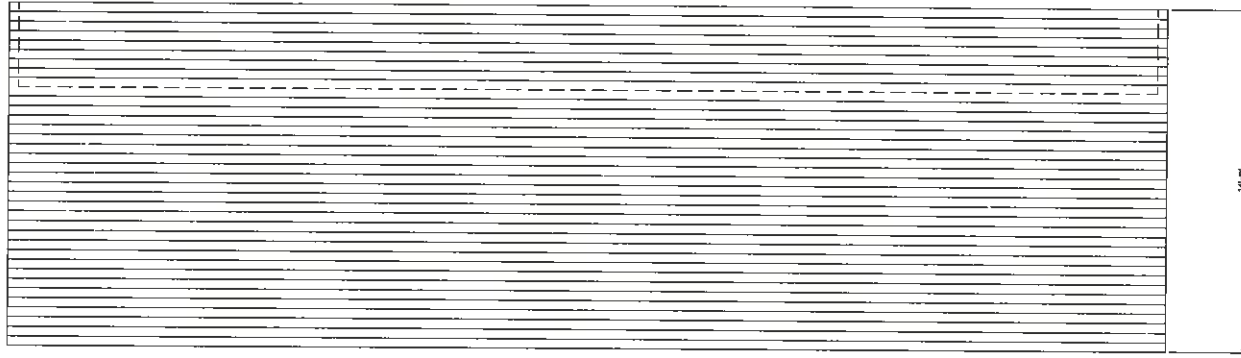
WENDELL W. VEITH

NEW BUILDING FOR:
CHUEY'S AUTO REPAIR
 5 ARCADE STREET, CATHEDRAL CITY, CA

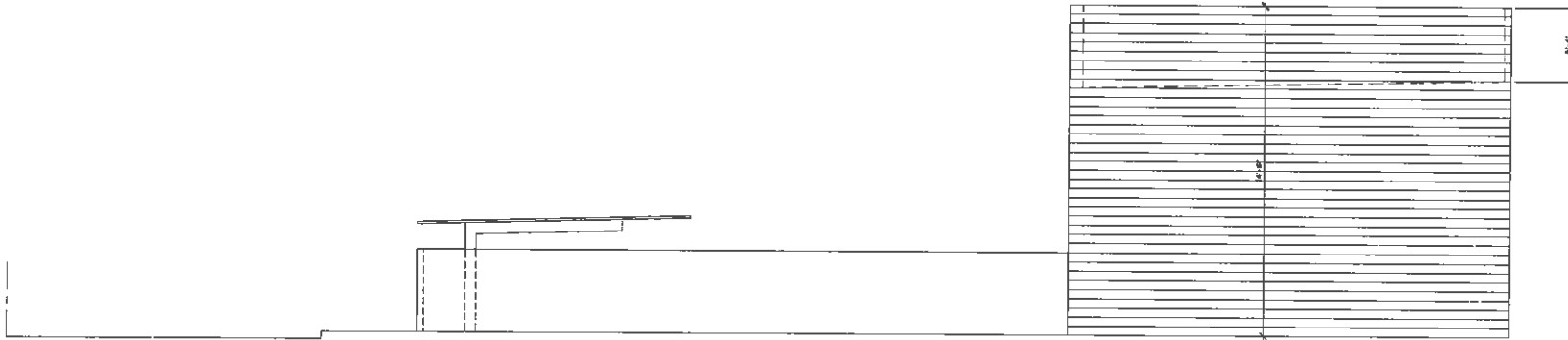
907-300 ULLSWATER DRIVE
 INDIO, CA. 92205
 TEL. 166-953-4856, FAX. 268-1602

DATE:
 FLOOR PLAN I
 STORAGE AREA PLAN

A1



WEST ELEVATION



NORTH ELEVATION



ARCHITECT

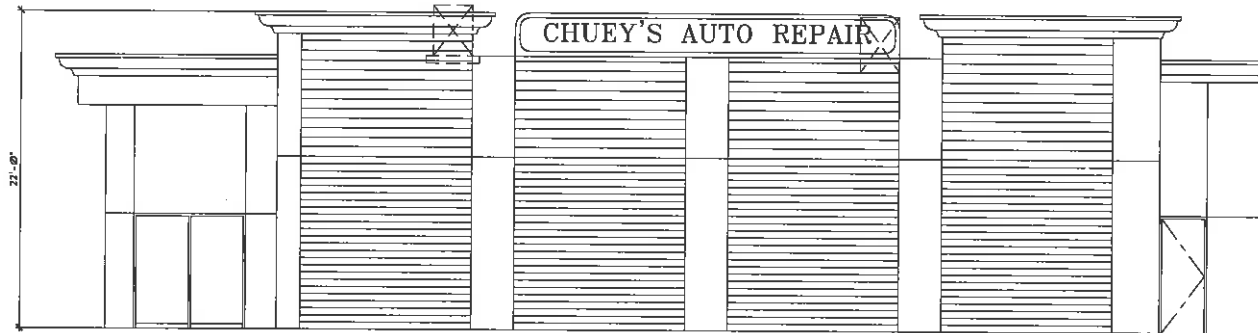
800-300 ULLSWATER DRIVE
INDIO, CA. 92203
TEL. 760-953-4856, FAX. 700-2602

WENDELL W. VEITH

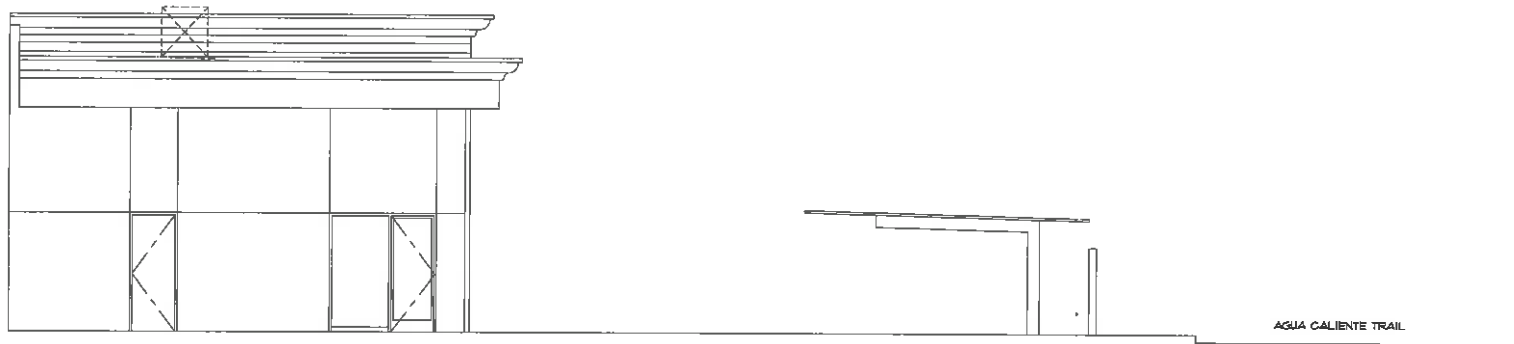
NEW BUILDING FOR
CHUEY'S AUTO REPAIR
413 W. SUNVIEW AVE., PALM SPRINGS, CA. 92267

EXTERIOR ELEVATIONS

A3



EAST ELEVATION



SOUTH ELEVATION



ARCHITECT

80-300 ULLSWATER DRIVE
INDIO, CA. 92203
T. 760-953-4556, F. 760-200-2607



W E N D E L L W. V E I T H

NEW BUILDING FOR:
CHUEY'S AUTO REPAIR
BARAH STREET, CATHEDRAL CITY, CA

EXTERIOR ELEVATIONS

DATE:

A2

WENDELL W. VEITH ARCHITECT #C5297

80-300 ULLSWATER DRIVE, INDIO, CA. 92203

TEL. 760-953-4556, FAX. 760-200-2602

www.wveith36@yahoo.com

PAUL RULL

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

RIVERSIDE, CA.

RE: ZAP1069PS18 AUTO REPAIR SHOP

PER YOUR REQUEST FOR AREAS OF THE PROPOSED BUILDING, THE AREAS ARE AS FOLLOWS:

1. REPAIR AREA: 1920 S.F.
2. OFFICE & TOILET ROOMS: 407 S.F.
3. STORAGE AREA FIRST FLOOR: 275 S.F.
4. STORAGE AREA MEZZANINE: 682 S.F.

PLEASE CALL IF YOU NEED FURTHER INFORMATION

WENDELL W. VEITH ARCHITECT #C5297

**NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Thursday from 8:00 a.m. to 4:30 p.m., except Monday, October 8 (Columbus Day), and by prescheduled appointment on Fridays, from 9:00 a.m. to 5:00 p.m.

ATTENTION: ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. The City of Cathedral City may hold hearings on this project and should be contacted on non-ALUC issues.

PLACE OF HEARING: Riverside County Administration Center
 4080 Lemon St., 1st Floor Board Chambers
 Riverside, California

DATE OF HEARING: October 11, 2018

TIME OF HEARING: 9:30 A.M.

CASE DESCRIPTION:

ZAP1069PS18 – J.J.L. Ibarra (Representative: Wendell Veith, Architect) – City of Cathedral City Planning Case No. CUP 18-009 (Conditional Use Permit). The applicant proposes to construct a 2,602 square foot automobile repair shop building on a 0.2-acre parcel located at the northwest corner of Sarah Street and Agua Caliente Trail (Airport Compatibility Zones C and D of the Palm Springs International Airport Influence Area).

FURTHER INFORMATION: Contact Paul Rull at (951) 955-6893. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Ms. Brenda Ramirez of the City of Cathedral City Planning Department at (760) 770-0339.



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP1069PS18 DATE SUBMITTED: 8/29/2018

APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

PS
C+D

Applicant	<u>J. J. L. BARRA</u>	Phone Number	
Mailing Address	<u>68235 TORTUGA RD CATHEDRAL CITY, CA 92234</u>	Email	
Representative	<u>WENDELL VECTA APOLLONIA</u>	Phone Number	<u>760-953-8556</u>
Mailing Address	<u>80-300 HILLSWATER DR INDIO, CA 92203</u>	Email	<u>wveith@lyonh.com</u>
Property Owner	<u>J. J. L. BARRA</u>	Phone Number	
Mailing Address		Email	

LOCAL JURISDICTION AGENCY

Local Agency Name	<u>CATHEDRAL CITY</u>	Phone Number	
Staff Contact	<u>BRENDA</u>	Email	
Mailing Address		Case Type	
Local Agency Project No	<u>CUP NO. 18-009</u>	<input type="checkbox"/> General Plan / Specific Plan Amendment <input type="checkbox"/> Zoning Ordinance Amendment <input type="checkbox"/> Subdivision Parcel Map / Tentative Tract <input checked="" type="checkbox"/> Use Permit <input type="checkbox"/> Site Plan Review/Plot Plan <input type="checkbox"/> Other	

PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address	<u>SARAH STREET 2 AQUA CALIENTE TRAIL</u>	Gross Parcel Size	<u>8794.5 sq</u>
Assessor's Parcel No.	<u>677-446-022</u>	Nearest Airport	
Subdivision Name	<u>PALM SPRINGS OUTPOST UNIT 5</u>	and distance from	
Lot Number	<u>202</u>	Airport	<u>9000 FT</u>

PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe)	<u>VACANT</u>

Proposed Land Use (describe)	AUTO REPAIR FACILITY		
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)	N/A	
For Other Land Uses (See Appendix C)	Hours of Operation		
	Number of People on Site	Maximum Number	
	Method of Calculation		
Height Data	Site Elevation (above mean sea level)	369.40 FT	ft.
	Height of buildings or structures (from the ground)	22'	ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?		<input type="checkbox"/> Yes
	If yes, describe		<input checked="" type="checkbox"/> No

- A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:**
1. Completed ALUC Application Form *
 1. ALUC fee payment
 1. Plans Package (24x36 folded) (site plans, floor plans, building elevations, landscaping plans, grading plans, subdivision maps)
 1. Plans Package (8.5x11) (site plans, floor plans, building elevations, landscaping plans, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
 1. CD with digital files of the plans (pdf)
 1. Vicinity Map (8.5x11)
 1. Detailed project description
 1. Local jurisdiction project transmittal
 3. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
 3. Gummed address labels of all surrounding property owners within a 300 foot radius of the project site (**only required if the project is scheduled for a public hearing Commission meeting**). If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10) with ALUC return address. *

* Projects involving heliports/helicopter landing sites will require additional noticing procedures.

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.2

HEARING DATE: October 11, 2018

CASE NUMBER: ZAP1044TH18 – Peter Solomon (Representative: The Altum Group)

APPROVING JURISDICTION: City of Coachella

JURISDICTION CASE NO: CZ18-09 (Change of Zone), CUP307 (Conditional Use Permit)

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends that the Commission find the proposed Change of Zone CONSISTENT with the 2005 Jacqueline Cochran Regional Airport Land Use Compatibility Plan as amended in 2006, and find the proposed Conditional Use Permit and Architectural Review CONSISTENT, subject to the conditions included herein.

PROJECT DESCRIPTION: The applicant proposes to construct five new industrial buildings and expand and change the use of one existing building on 10.0 acres. The applicant has identified tenants who plan to utilize the existing building and building 1 for cannabis cultivation operations. Tenants have not been identified for buildings 2 through 5. The applicant also proposes to change the zoning of the 10-acre project site from Heavy Industrial (M-H) to Manufacturing Services – Industrial Park Overlay Zone (MS-IP). No dispensaries are proposed.

PROJECT LOCATION: The project site is located on the northeast corner of Avenue 54 and Polk Street in the City of Coachella, approximately 6,845 feet northeasterly of the northerly terminus of Runway 17-35 at Jacqueline Cochran Regional Airport.

LAND USE PLAN: 2005 Jacqueline Cochran Regional Airport Land Use Compatibility Plan (last amended in 2006)

- a. Airport Influence Area: Jacqueline Cochran Regional Airport
- b. Land Use Policy: Airport Compatibility Zone D
- c. Noise Levels: Below 55 CNEL from aircraft at this location

BACKGROUND:

Change of Zone: The applicant proposes to change the zoning of the property from Heavy Industrial (M-H) to Manufacturing Services – Industrial Park Overlay Zone (MS-IP). The applicant is requesting this change of zoning because the proposed cannabis cultivation and manufacturing uses are not permitted in the M-H zoning district. The MS-IP zone provisions were recently amended to allow cannabis-related land uses with a Conditional Use Permit.

The proposed M-S zone does allow retail and restaurant type uses, which could potentially exceed the Compatibility Zone D intensity criteria. However, such uses are not proposed through this project, nor have they been evaluated by ALUC staff. A condition is recommended specifying that uses be limited to offices, manufacturing, cultivation, storage, and warehousing. More intense uses such as retail sales or restaurants would require subsequent evaluation as to airport land use compatibility.

Non-Residential Average Intensity: Compatibility Zone D limits average intensity to 100 people per acre.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, the following rates were used to calculate the occupancy for the proposed buildings:

- Manufacturing – 1 person per 200 square feet,
- Office – 1 person per 200 square feet, and

Based on the project application, only the existing building and Building 1 are presently earmarked for cannabis cultivation and related uses, whereas the other buildings are speculative and do not have tenants at this time. Therefore, staff has evaluated buildings 2 through 5 on the basis of office/industrial manufacturing use at a potential occupancy of 1 person per 200 square feet. Uses with occupancies greater than one person per 200 square feet pursuant to the California Building Code should not be established therein without subsequent airport land use compatibility review.

The project proposes a total of 155,840 square feet of building area on 10.0 acres, consisting of 5 new buildings and one existing reuse (and expanded) building in four phases. If one assumed that all building area were to be occupied on the industrial manufacturing/office basis of 1 person per 200 square feet, the site would accommodate 779 people, for an average intensity of 78 people per acre, which would be consistent with the Zone D average criterion of 100 people per acre.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle in the absence of more precise data). Based on the number of parking spaces provided of 252, the total occupancy would be 378 people for an average intensity of 38 persons per acre, which would also be consistent with the Zone D average intensity criterion.

Any future intensive use within these buildings (uses with more than one person per 200 square feet, other than offices), such as retail stores or restaurants, would require subsequent compatibility evaluation.

Non-Residential Single-Acre Intensity: Compatibility Zone D limits single-acre intensity to 300 people.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre area would consist of 43,560 square feet of industrial manufacturing/office area, resulting in a single acre occupancy of 218 people, which is consistent with the Compatibility Zone D single-acre criterion of 300.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in D (highly noise-sensitive outdoor non-residential uses, and hazards to flight) within the project.

Noise: The property lies outside the areas subject to average exterior noise levels greater than 55 CNEL. Therefore, no special measures to mitigate aircraft-generated noise are required.

Part 77: The elevation of Runway 18-36 at its northerly terminus is approximately -114 feet (i.e., 114 feet below mean sea level). At a distance of approximately 6,845 feet from the runway to the project site, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding -45 feet mean sea level (MSL). The site elevation is approximately -100 feet MSL, and their maximum building height is 30 feet, for a top point elevation of -70 feet MSL. Therefore, review of buildings by the FAA Obstruction Evaluation Service (FAA OES) is not required.

Open Area: Zone D requires a minimum of 10% of open area for projects 10 acres or larger be set aside as open area that could potentially serve as emergency landing areas. As a 10-acre site in Zone D, the project is required to provide a minimum of 1.0 acre of open area consistent with ALUC open area criteria. The applicant has provided 1.0 acre of open area located within the drive aisles and parking areas of the project as shown on the site plan. These areas are to remain as open space and the project is conditioned to maintain at least 1.0 acre of this area consistent with ALUC open area requirements of 300 feet by 75 feet minimum shape, and prohibit obstructions greater than 4 feet in height that are at least 4 inches in diameter.

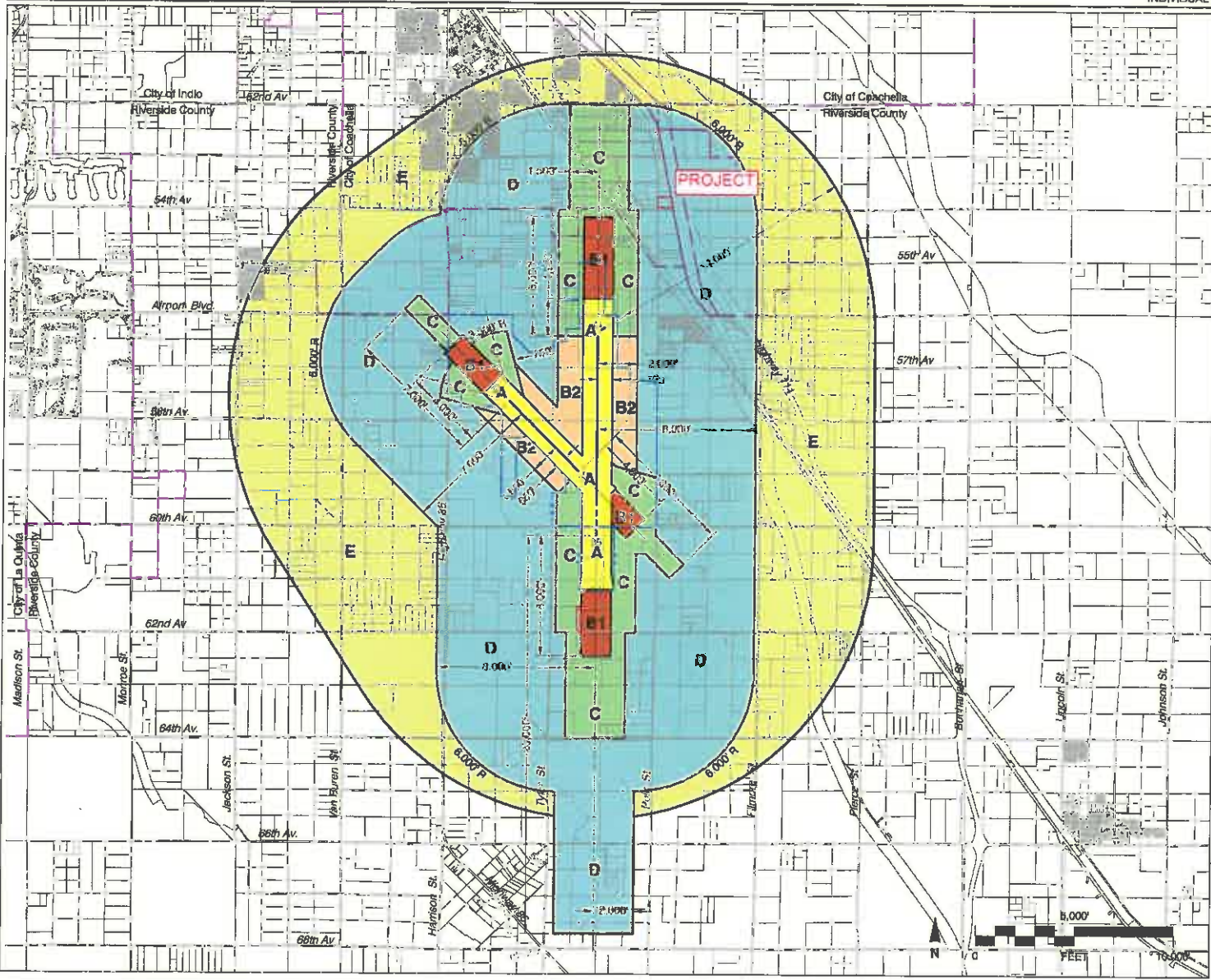
CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting plans, if any, shall be transmitted to Riverside County Economic Development Agency – Aviation Division personnel and to the Jacqueline Cochran Regional Airport for review and comment. (Failure to comment within thirty days shall be considered to constitute acceptability on the part of the airport manager.)

2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations, or any type of strobe light, toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, livestock operations, production of cereal grains, sunflower, and row crops, artificial marshes, wastewater management facilities, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Highly noise-sensitive outdoor non-residential uses and hazards to flight.
3. The attached notice shall be provided to all prospective purchasers of the property and tenants of the buildings thereon, and shall be recorded as a deed notice.
4. Any new detention basin(s) shall be designed so as to provide a maximum 48-hour detention period for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basin that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
5. The proposed buildings were evaluated on the basis of office and manufacturing. No retail uses, restaurants, dispensaries or other uses with Building Code occupancy levels greater than one person per 200 square feet (other than offices) are authorized without subsequent airport land use compatibility evaluation.
6. The ALUC eligible open areas shown on the open space exhibit (minimum 1.0 acre) shall be kept obstacle and obstruction free per ALUC open area definition.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Legend

- Compatibility Zones**
- Airport Influence Area Boundary
 - Zone A
 - Zone B1
 - Zone B2
 - Zone C
 - Zone D
 - Zone E

- Boundary Lines**
- Airport Property Line - Existing
 - Airport Property Line - Planned
 - City Limits

Note
 Except for southern extension, Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

See Chapter 2, Table 2A for compatibility criteria associated with this map.

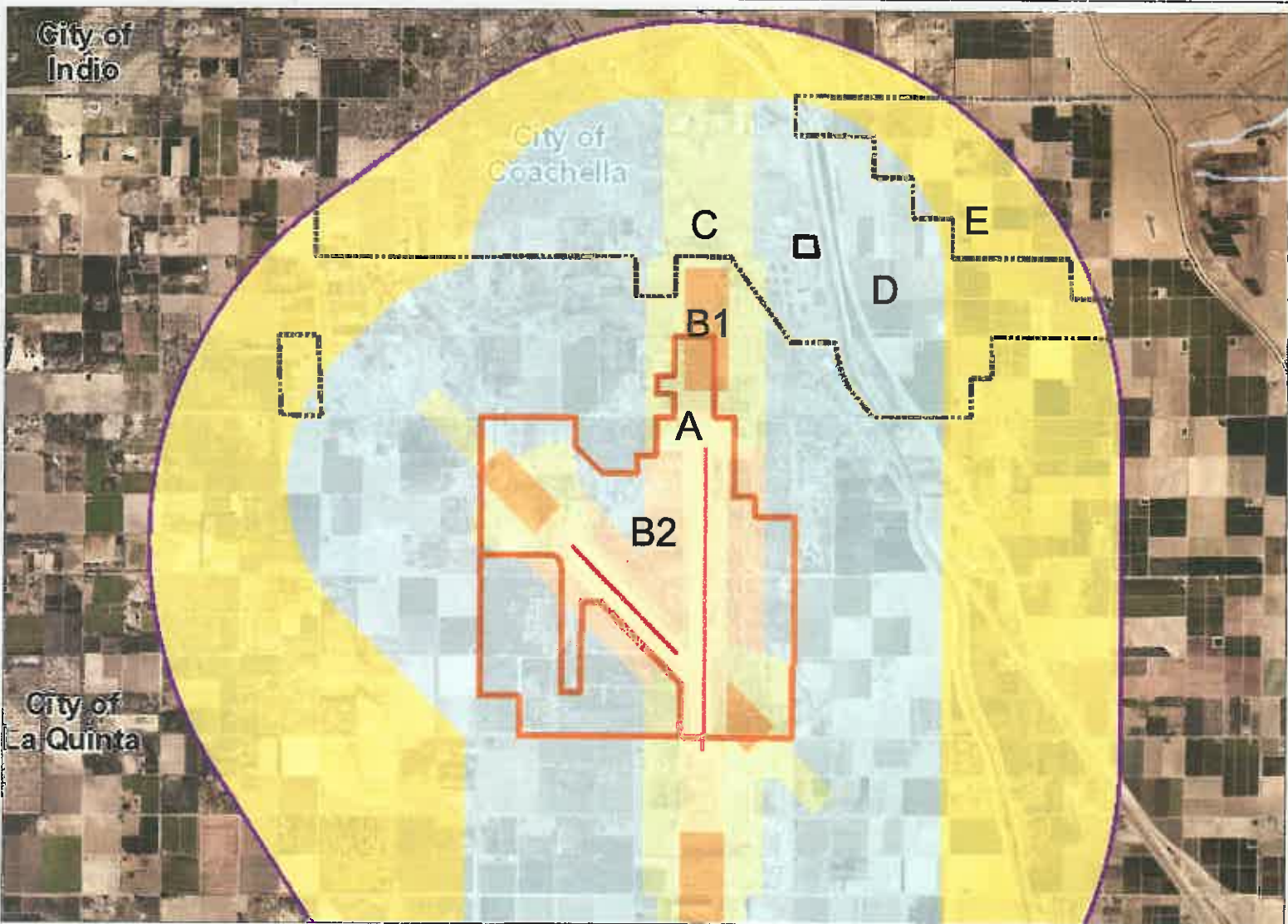
**Riverside County
 Airport Land Use Commission
 Riverside County
 Airport Land Use Compatibility Plan
 Policy Document
 (Adopted June 2005)**

Map JC-1

Compatibility Map
 Jacqueline Cochran Regional Airport



Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6

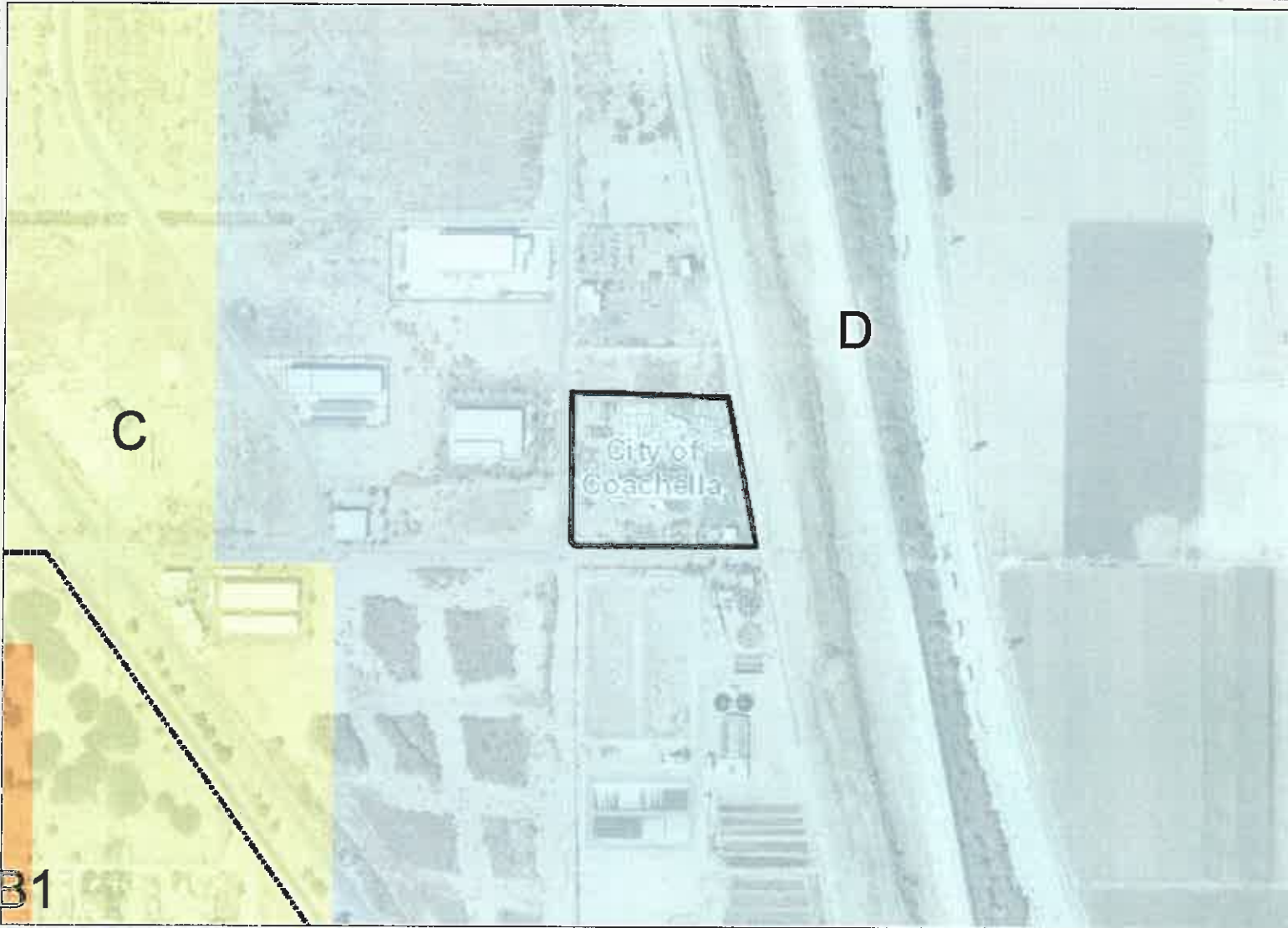


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Notes



Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC8



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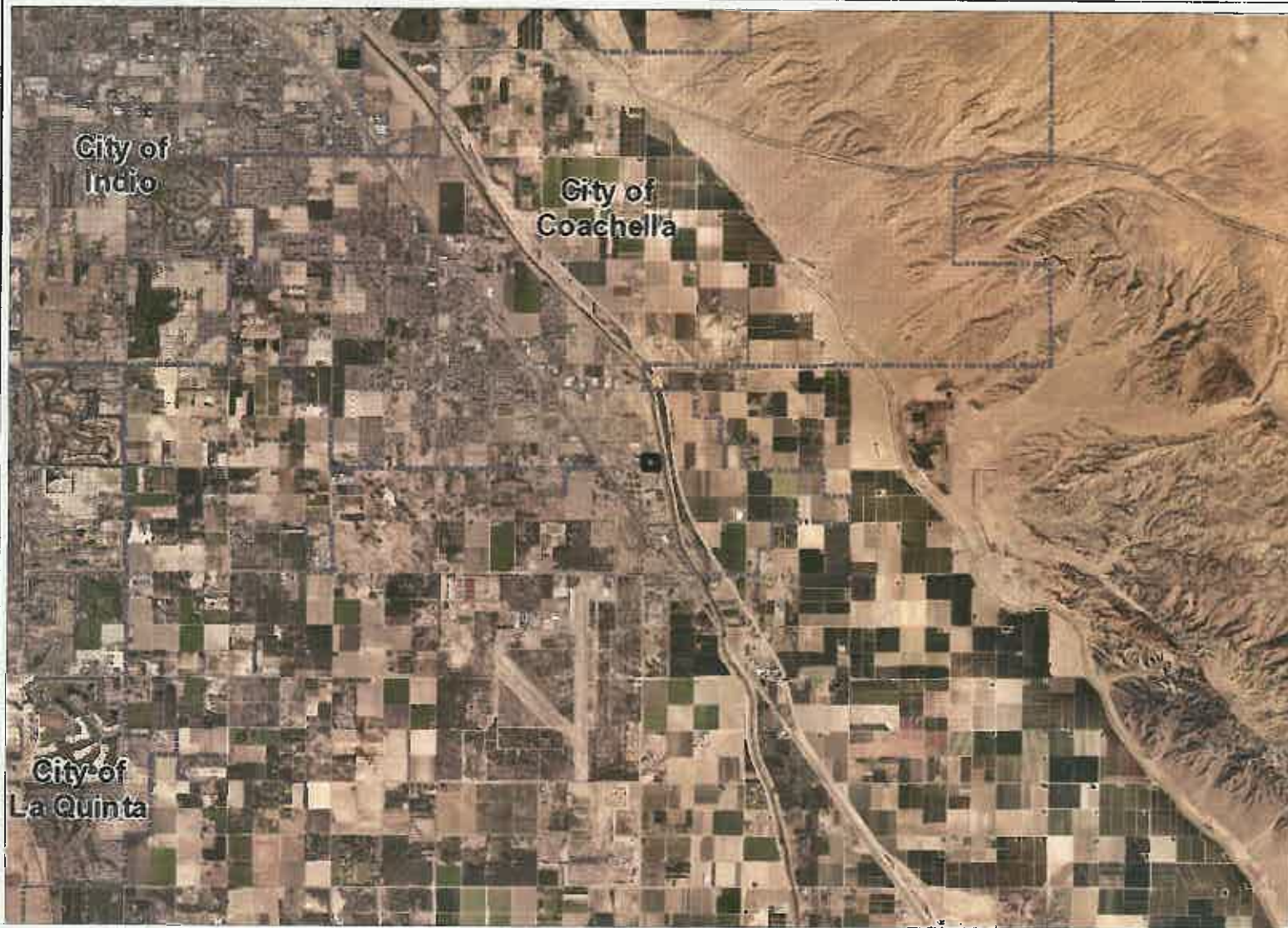
Notes

0 770 1,539 Feet

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Map My County Map



Legend

-  City Areas
-  World Street Map



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Notes



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Legend

- Blueline Streams
- City Areas
- World Street Map



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Notes

0 4 9,440 Feet
720

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- Legend**
- Blue line Streams
 - City Areas
 - World Street Map



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Notes

Map My County Map



Legend

- Blueline Streams
- ▣ City Areas
- World Street Map



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Notes

0 770 1,539 Feet

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Map My County Map



Legend

-  Parcels
-  Blueline Streams
-  City Areas
-  World Street Map



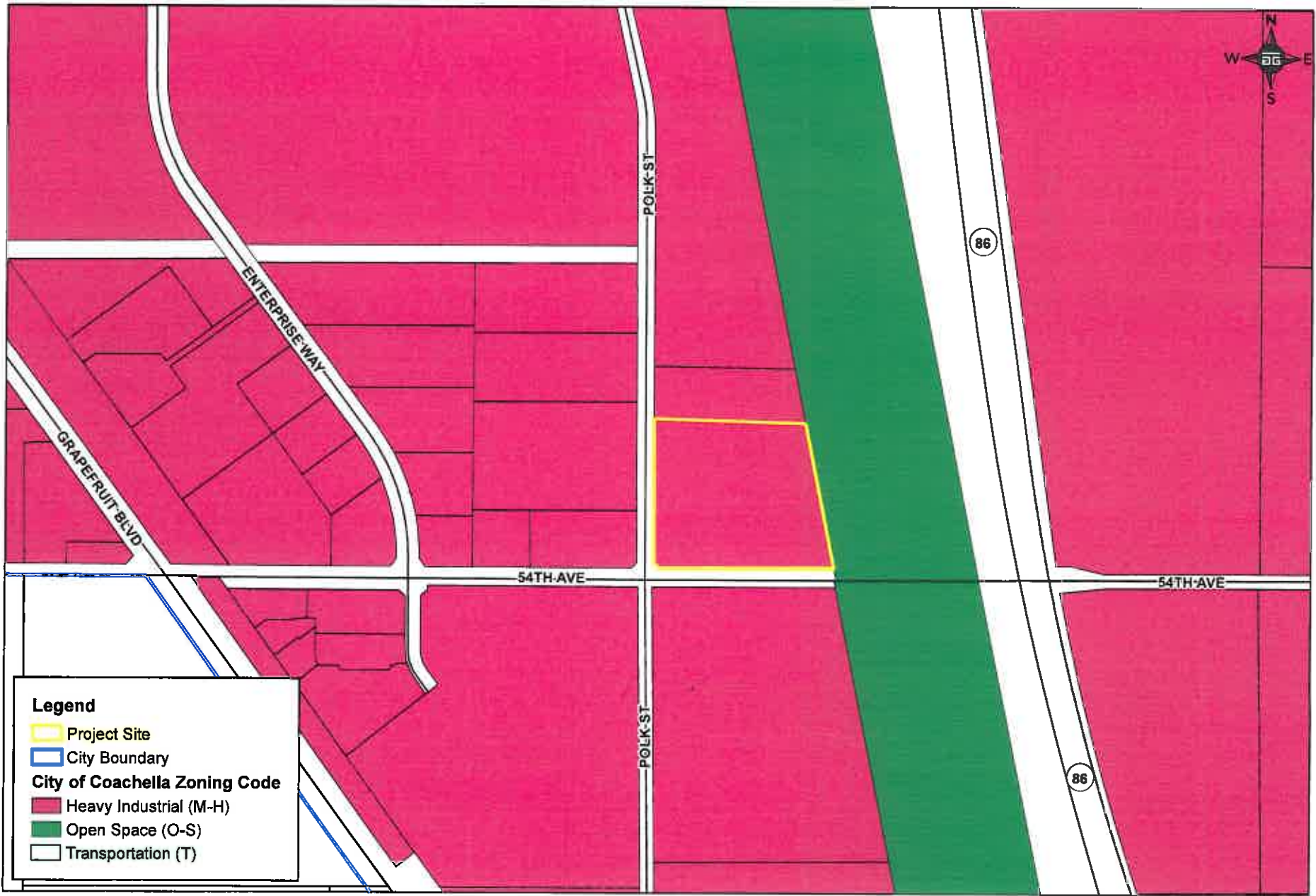
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

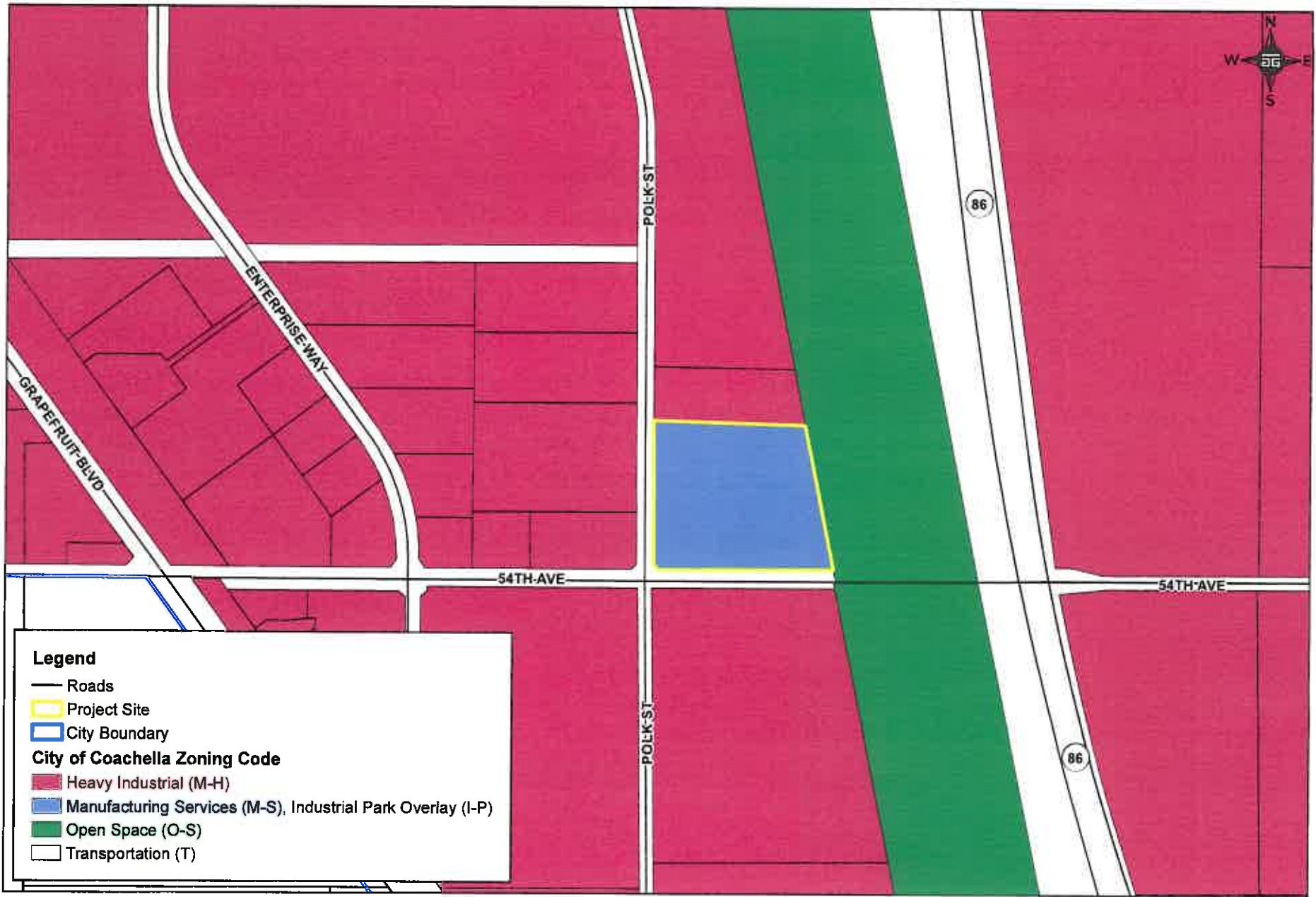
Notes

0 590 1,180 Feet

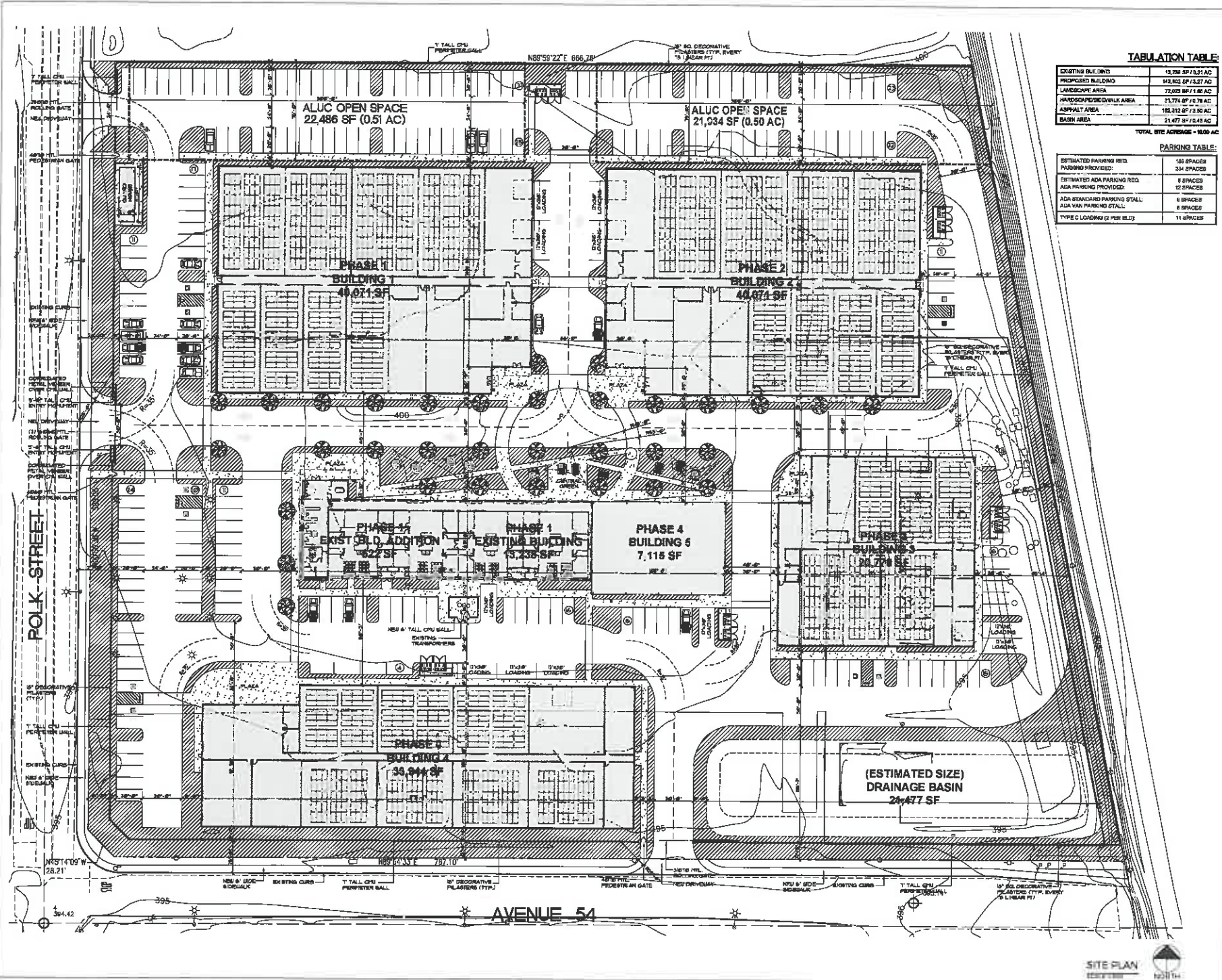
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1 in = 560 ft



TABULATION TABLE:

EXISTING BUILDING	13,335 SF / 0.31 AC
PROPOSED BUILDING	143,822 SF / 3.27 AC
LANDSCAPE AREA	72,023 SF / 1.64 AC
HARDSCAPE/BIOPAVK AREA	31,774 SF / 0.73 AC
ASPHALT AREA	160,312 SF / 3.66 AC
BASIN AREA	21,477 SF / 0.48 AC
TOTAL SITE ACREAGE = 10.00 AC	

PARKING TABLE:

ESTIMATED PARKING REQ.	160 SPACES
PARKING PROVIDED	334 SPACES
ESTIMATED ADA PARKING REQ.	8 SPACES
ADA PARKING PROVIDED	12 SPACES
ADA STANDARD PARKING STALL	8 SPACES
ADA VAN PARKING STALL	4 SPACES
TYPE C LOADING (2 PER BLD.)	11 SPACES



75-115 Calle Estado, Ste. 105
La Quinta, CA 92253
P. (760) 364-4707
F. (760) 364-4788
www.sw2cpl.com

Cannabis Redevelopment
53 800 Polk Street
Cochella, CA 92236

Date / Description

Number

Drawn By
Stephen N.

Project Manager

Approved By

MASTER SITE PLAN



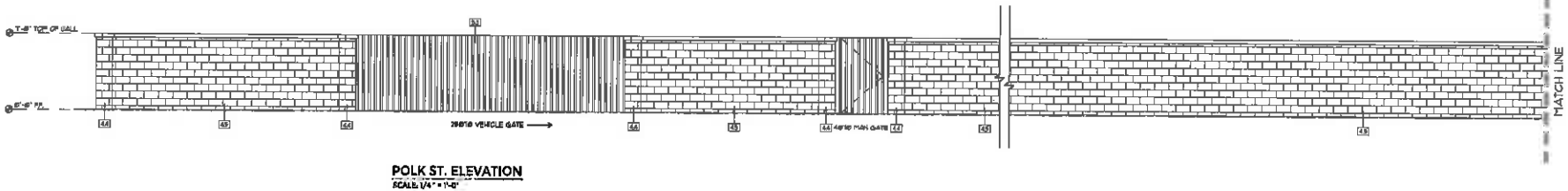
DATE: 02/27/2018

A1.1

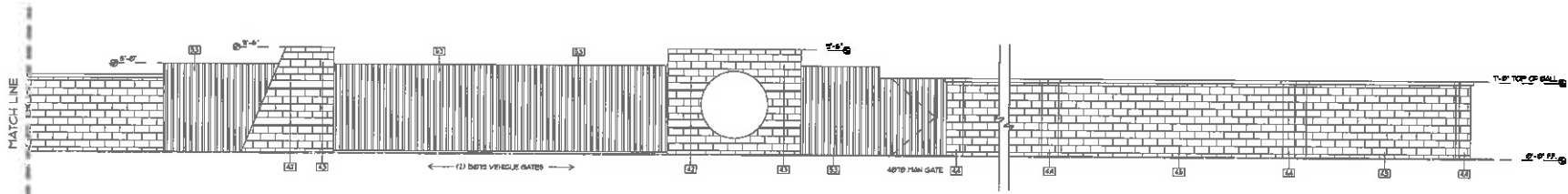
**EXTERIOR ELEVATIONS
REFERENCE NOTES**

- | | | |
|---|--|--|
| <p>4.00 MASONRY</p> <p>01 8" Precast CMU wall w/ smooth cast stucco plaster finish
Finish by TBD
Color: TBD</p> <p>02 8" Split Face CMU wall
Finish by TBD
Color: TBD</p> <p>03 8" Brick Blank CMU wall
Finish by TBD
Color: TBD</p> <p>04 8" Decorative pavers (TYP)
Finish by TBD
Color: TBD</p> <p>05 8" Precast CMU wall
Finish by TBD
Color: TBD</p> | <p>5.00 METALS</p> <p>06 2x10' Up. Pig metal
Finish by TBD
Color: TBD</p> <p>07 2" Metal composite cap by MGC-CLAD
Finish by TBD
Color: TBD</p> <p>08 Architectural Metal Panel
Finish by TBD
Color: TBD</p> <p>09 Metal Cladding with overhead horizontal support
Finish by TBD
Color: TBD</p> <p>10 Painted aluminum signposts with 6" letter caps
Finish by TBD
Color: TBD</p> <p>11 Galvalume corrugated metal exterior
Finish by TBD
Color: TBD</p> <p>12 Non-Architectural screen over existing corrugated metal exterior
Finish by TBD
Color: TBD</p> <p>13 Sliding metal structure</p> | <p>6.00 WOOD & PLASTICS</p> <p>14 3/4" solid oak w/ 1/4" OC w/ 1/2" exterior smooth finish plaster.
Finish by TBD
Color: TBD</p> |
|---|--|--|

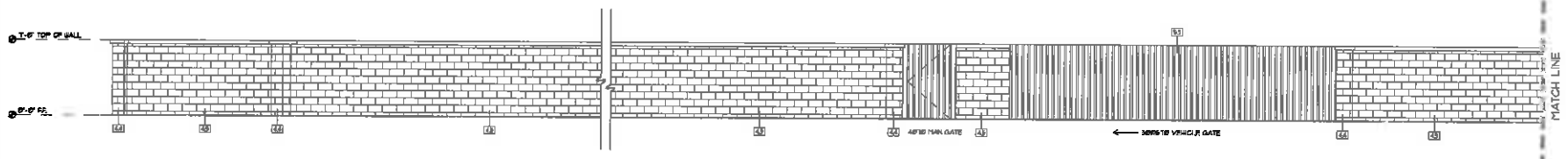
- 7.00 OPENINGS
- 15 Aluminum Frame Sliding by TBD
Color: TBD
- 16 Painted Metal Exterior Door and Frame
Color: TBD
- 17 Non-Roll Up Metal Garage door 14' x 14'
Finish by TBD
Color: TBD
- 18 Sliding Roll Up Metal Garage door 12'-6" x 14'
Finish by TBD
Color: TBD



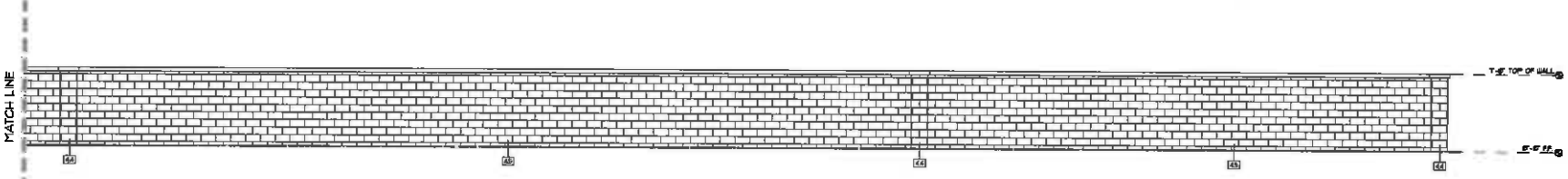
POLK ST. ELEVATION
SCALE: 1/4" = 1'-0"



POLK ST. ELEVATION MAIN ENTRY
SCALE: 1/4" = 1'-0"



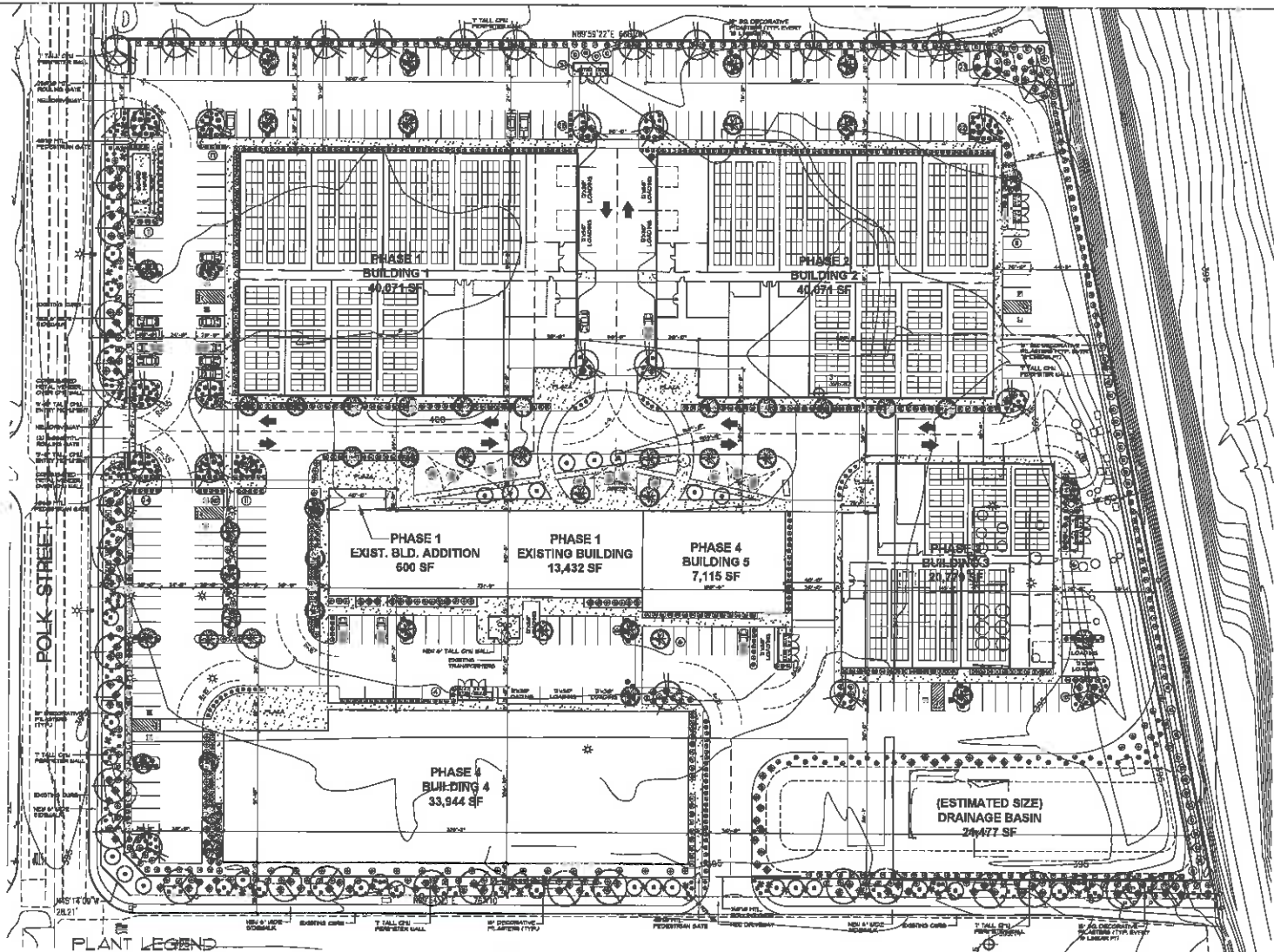
AVE. 54 ELEVATION
SCALE: 1/4" = 1'-0"



AVE. 54 ELEVATION
SCALE: 1/4" = 1'-0"

Date / Description _____
Number _____
Drawn By: _____
Stephen N. _____
Project Manager _____
Approved By: _____





PLANT LEGEND

TREES & PALMS

SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE	REMARKS	WATER USE
	ACACIA ANURA	"TULGA"	24" 50X	SINGLE TRUNK P CALIPER EVERGREEN PLANTURE HT. 30'	03 LOW
	ACACIA SYNOPHYLLA	SHADESTRONG ACACIA	24" 50X	SINGLE TRUNK P CALIPER EVERGREEN PLANTURE HT. 30'	02 LOW
	PITHECOLLOBIUM FLEXIBILE	TEXAS EBONY	14" 50X	SINGLE TRUNK P CALIPER NEVER EVERGREEN PLANTURE HT. 30'	03 MED
	WASHINGTONIA HYBRID		1' 6TH	ROUND EVERGREEN	03 MED

SHRUBS & CACTII

SYMBOL	BOTANICAL NAME	COMMON NAME	WATER USE
	LEUCOPHYLLUM "LYNNS LEGACY"	LYNNS LEGACY	9 GAL. 03 LOW
	LEUCOPHYLLUM LANGHANSIAE	RED BRAVO	9 GAL. 02 LOW
	CALLISTEMON H. VIMINALIS	LITTLE JOHN BOTTLEBRUSH	8 GAL. 03 MED
	LEUCOPHYLLUM MUTISCENS	TEXAS RANGER	9 GAL. 03 LOW
	CARISSA MACROCARPA	NATAL PLUM	5 GAL. 02 LOW
	AGAVE AMERICANA	CENTURY PLANT	8 GAL. 02 LOW
	HESPERALOE PARVIFLORA "BRAKELIGHT"		8 GAL. 02 LOW
	SENECIO NEMOPHILA	OBSESS CLASSIA	9 GAL. 02 LOW
	BOUGAINVILLEA	ROSEEGE	9 GAL. 03 MED
	CARISSA MACROCARPA	TUTTLEI	9 GAL. 03 MED
	LANTANA	NEW GOLD	9 GAL. 03 MED

POLK STREET BUSINESS PARK



HERMANN DESIGN GROUP
 77459 WOLF RD.
 SUITE 102
 P.O. BOX 102
 92211
 LINDSEY, CALIFORNIA 92551
 TEL: (760) 777-0131
 FAX: (760) 777-0132

DATE: 8/27/08

NOTICE OF PUBLIC HEARING

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Thursday from 8:00 a.m. to 4:30 p.m., except Monday, October 8 (Columbus Day), and by prescheduled appointment on Fridays, from 9:00 a.m. to 5:00 p.m.

ATTENTION: ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. The City of Coachella will hold hearings on this project and should be contacted on non-ALUC issues.

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon St, 1st Floor Board Chambers
Riverside, California

DATE OF HEARING: October 11, 2018

TIME OF HEARING: 9:30 A.M.

CASE DESCRIPTION:

ZAP1044TH18 – Peter Solomon (Representative: The Altum Group) – City of Coachella Planning Case Nos. CZ18-09 (Change of Zone), CUP307 (Conditional Use Permit). The applicant proposes to construct five new industrial buildings and expand and change the use of one existing building on 10.0 acres located on the northeast corner of Avenue 54 and Polk Street. The applicant has identified tenants who plan to utilize the existing building and building 1 for cannabis cultivation operations. Tenants have not been identified for buildings 2 through 5. The applicant also proposes to change the site's zoning from Heavy Industrial (M-H) to Manufacturing Services – Industrial Park Overlay Zone (MS-IP). No dispensaries are proposed. (Airport Compatibility Zone D of the Jacqueline Cochran Regional Airport Influence Area)

FURTHER INFORMATION: Contact Paul Rull at (951) 955-6893. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Mr. Luis Lopez of the City of Coachella Planning Department at (760) 398-3102.



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP1049TH18 DATE SUBMITTED: August 29, 2018

APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

JCRA
D

Applicant	Peter Solomon	Phone Number 760-578-0943
Mailing Address	53-800 Polk Street, Coachella CA 92236	Email petersolomon@regencyhomes.co
Representative	Audrey Nickerson	Phone Number (760) 346-4750
Mailing Address	The Altum Group	Email audrey.nickerson@thealtumgroup.com
Property Owner	Peter Solomon	Phone Number 760-578-0943
Mailing Address	53-800 Polk Street, Coachella CA 92236	Email petersolomon@regencyhomes.co

LOCAL JURISDICTION AGENCY

Local Agency Name	City of Coachella	Phone Number (760) 398-3102
Staff Contact	Luis Lopez	Email llopez@coachella.org
Mailing Address	1515 Sixth Street, Coachella CA 92236	Case Type
		<input type="checkbox"/> General Plan / Specific Plan Amendment <input checked="" type="checkbox"/> Zoning Ordinance Amendment <input type="checkbox"/> Subdivision Parcel Map / Tentative Tract <input type="checkbox"/> Use Permit <input type="checkbox"/> Site Plan Review/Plot Plan <input checked="" type="checkbox"/> Other Conditional Use Permit
Local Agency Project No		

PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address	53-800 Polk Street, Coachella CA 92236	
Assessor's Parcel No.	763-160-012	Gross Parcel Size 10.0 acres
Subdivision Name		Nearest Airport and distance from Airport
Lot Number		Jaqueline Cochran; 1.25 miles northeast

PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe)	Zoning Designation: M-H Heavy Industrial; site currently operated with various industrial land uses. One existing industrial warehouse is located in the center of the property. Approximately 60% of the site is covered in asphalt concrete and the remainder of graded dirt. The project site is developed as multi-use industrial with several storage, distribution, contractor staging, equipment type uses, and agricultural chemical storage, mixing and servicing use which are regulated for generating, storing, or disposing of hazardous materials or petroleum products. The Whitewater Channel is directly east of the project site and existing industrial uses are located adjacent to the site.
------------------------------	---

Proposed Land Use (describe)	The proposed project includes development of a 10-acre industrial site (proposed project) for construction of Cannabis-related land uses, a potential Commercial Graphic Film Studio, and various other permitted commercial and industrial land uses. The project would be developed in four phases that would be constructed over the course of 8 to 10 years with a build-out maximum of 174,500 SF.		
	The proposed project will require a Change of Zone (CZ) from the existing M-H (Heavy Industrial) to Manufacturing Service – Industrial Park Overlay (MS-IP). The zone change will create consistency with the existing general plan land use designation. See the detailed project description included in the justification letter for more information.		
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)		
For Other Land Uses (See Appendix C)	Hours of Operation	Cultivation: 24 hrs/day, 7 days/week; Manufacturing: 14 hrs/day, 6 days/week; Film Studio: 12 hrs/day, 5 days/week (dark when no project)	
	Number of People on Site	345	Maximum Number 873
	Method of Calculation	Generally, light industrial uses proposed onsite = 1employee/200 SF; max 174,500 SF onsite building area Max Total: 872.5; Average (per acre) = 87; Max (per acre) = 178	
Height Data	Site Elevation (above mean sea level)	-100 (100 ft bmsl)	ft.
	Height of buildings or structures (from the ground)	max 30 feet	ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?		<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
	If yes, describe _____ _____ _____		

- A. **NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. **REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. **SUBMISSION PACKAGE:**
1. Completed ALUC Application Form
 1. ALUC fee payment
 1. Plans Package (24x36 folded) (site plans, floor plans, building elevations, landscaping plans, grading plans, subdivision maps)
 1. Plans Package (8.5x11) (site plans, floor plans, building elevations, landscaping plans, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
 1. CD with digital files of the plans (pdf)
 1. Vicinity Map (8.5x11)
 1. Detailed project description
 1. Local jurisdiction project transmittal
 3. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
 3. Gummed address labels of all surrounding property owners within a 300 foot radius of the project site (**only required if the project is scheduled for a public hearing Commission meeting**). If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10) with ALUC return address. *

* Projects involving heliports/helicopter landing sites will require additional noticing procedures.

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.3

HEARING DATE: October 11, 2018

CASE NUMBER: ZAP1031BA18 – Diana and John Hanna

APPROVING JURISDICTION: City of Banning

JURISDICTION CASE NOS: GPA18-2504 (General Plan Amendment, ZC18-3503 (Zone Change), DR18-7011 (Design Review)

MAJOR ISSUES: As of the date of preparation of this staff report, staff has not been provided evidence that the Federal Aviation Administration Obstruction Evaluation Service (FAA OES) is actively processing the applicant’s submittal, a requirement due to the site’s location at an elevation more than 130 feet above that of the airport runway.

RECOMMENDATION: Staff recommends that the Commission find the proposed General Plan Amendment and Change of Zone CONSISTENT with the 2004 Banning Municipal Airport Land Use Compatibility Plan as amended in 2016, and that the Commission CONTINUE its consideration of the Design Review case to its meeting of November 8, 2018. However, if, prior to the October 11 hearing, the FAA OES website reveals that the submittal is in a “Work in Progress” status, staff will recommend that the Commission find the Design Review CONDITIONALLY CONSISTENT, subject to the conditions included herein and such additional conditions as may be necessary to meet the requirements of the Federal Aviation Administration Obstruction Evaluation Service.

PROJECT DESCRIPTION: Design Review Case No. DR18-7011 is a proposal to develop a 96-unit apartment complex with clubhouse on 7.08 to 7.34 acres (Assessor’s Parcel Numbers 534-283-011 and 534-283-014). The proposed project requires a general plan amendment and a zone change from Low Density Residential to High Density Residential.

PROJECT LOCATION: The site is located westerly of Hathaway Street and southerly of Hoffer Street (northerly of an existing apartment complex) in the City of Banning, approximately 3,611 feet northerly of the westerly terminus of Runway 8-26 at Banning Municipal Airport.

LAND USE PLAN: 2004 Banning Municipal Airport Land Use Compatibility Plan, as amended in 2016

a. Airport Influence Area: Banning Municipal Airport

b. Land Use Policy: Airport Compatibility Zone D

c. Noise Levels: Below 55 CNEL

BACKGROUND:

Residential Density: The site is located in Compatibility Zone D of the Banning Municipal Airport Influence Area. Compatibility Zone D limits residential density to either urban densities of five or more dwelling units per acre or rural densities of 0.2 dwelling units per acre or less (essentially a five-acre minimum lot size). The proposed project has a density of 13.1 - 13.6 dwelling units per acre and is, therefore, consistent with Zone D criteria for urban density housing.

Nonresidential Intensity: The project design includes one nonresidential building, a clubhouse. Based on the site plan, it appears that the clubhouse is approximately 1,768 square feet in area and would accommodate a capacity of 118 persons if in use as dining area or 253 persons if used as an assembly hall. These intensity levels are consistent with the Zone D single-acre intensity limit of 800 persons in any given acre.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zone D (children's schools, libraries, hospitals, nursing homes, highly noise-sensitive outdoor nonresidential uses, and hazards to flight) within the project.

Noise: The Banning Municipal Airport Land Use Compatibility Plan depicts the site as being located outside the 55 CNEL contour range from aircraft noise. Therefore, no special noise attenuation measures are required for this residential land use.

Part 77: The elevation of Runway 8-26 at its westerly terminus is 2,212 feet above mean sea level (2212 AMSL). At a distance of approximately 3,611 feet from the runway to the southerly boundary of the site, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with top of roof exceeding 2,248 feet AMSL. The proposed finish floor elevations range from 2,343 feet to 2,359 feet AMSL. With a maximum building height of 33 feet, the top point elevation could be as high as 2,392 feet AMSL. Therefore, review of buildings by the FAA OES is required. The applicant has been advised of this requirement.

Open Area: There is no applicable open area requirement, as the total area of the two properties does not exceed ten acres.

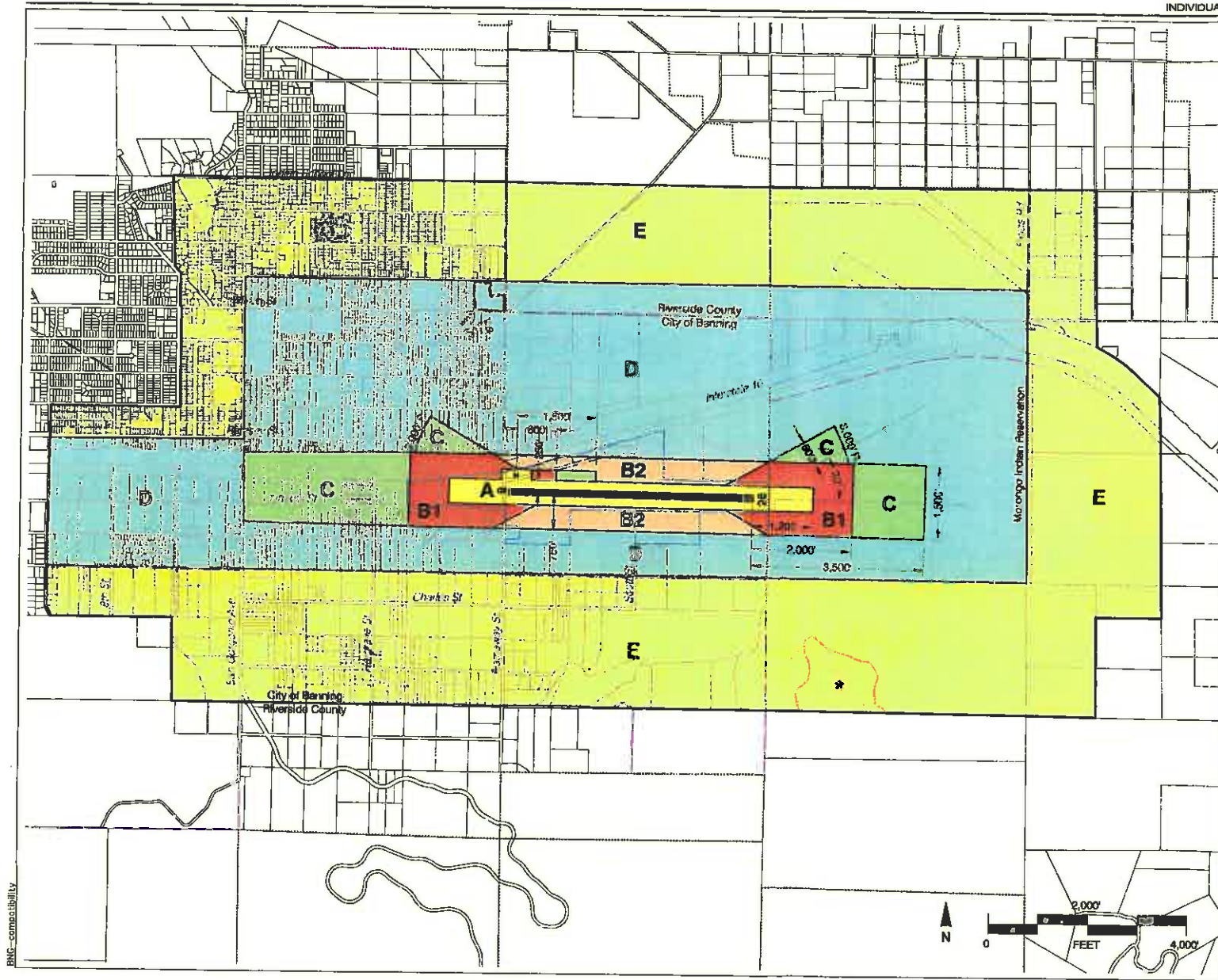
General Plan Amendment/Change of Zone: The proposed General Plan Amendment and Change of Zone would allow for the proposed development. Given the location of the site within Compatibility Zone D, the proposed amendment from Low Density Residential to High Density Residential would provide for a designation that is more consistent with the Compatibility Plan than the existing designation and zoning.

CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, artificial marshes, wastewater management facilities, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached notice shall be provided to all potential purchasers of the property and to tenants of the proposed dwelling units, and shall be recorded as a deed notice.
4. Any proposed detention basin or facilities shall be designed so as to provide for a detention period for the design storm that does not exceed 48 hours and to remain totally dry between rainfalls. Vegetation in and around such facilities that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
5. The proposed clubhouse shall be single-story and not exceed 1,768 square feet in floor area, unless additional airport land use compatibility evaluation occurs.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Legend

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E
- Height Review Overlay Zone

Boundary Lines

- Airport Property Line
- City Limits
- Morongo Indian Reservation

Note

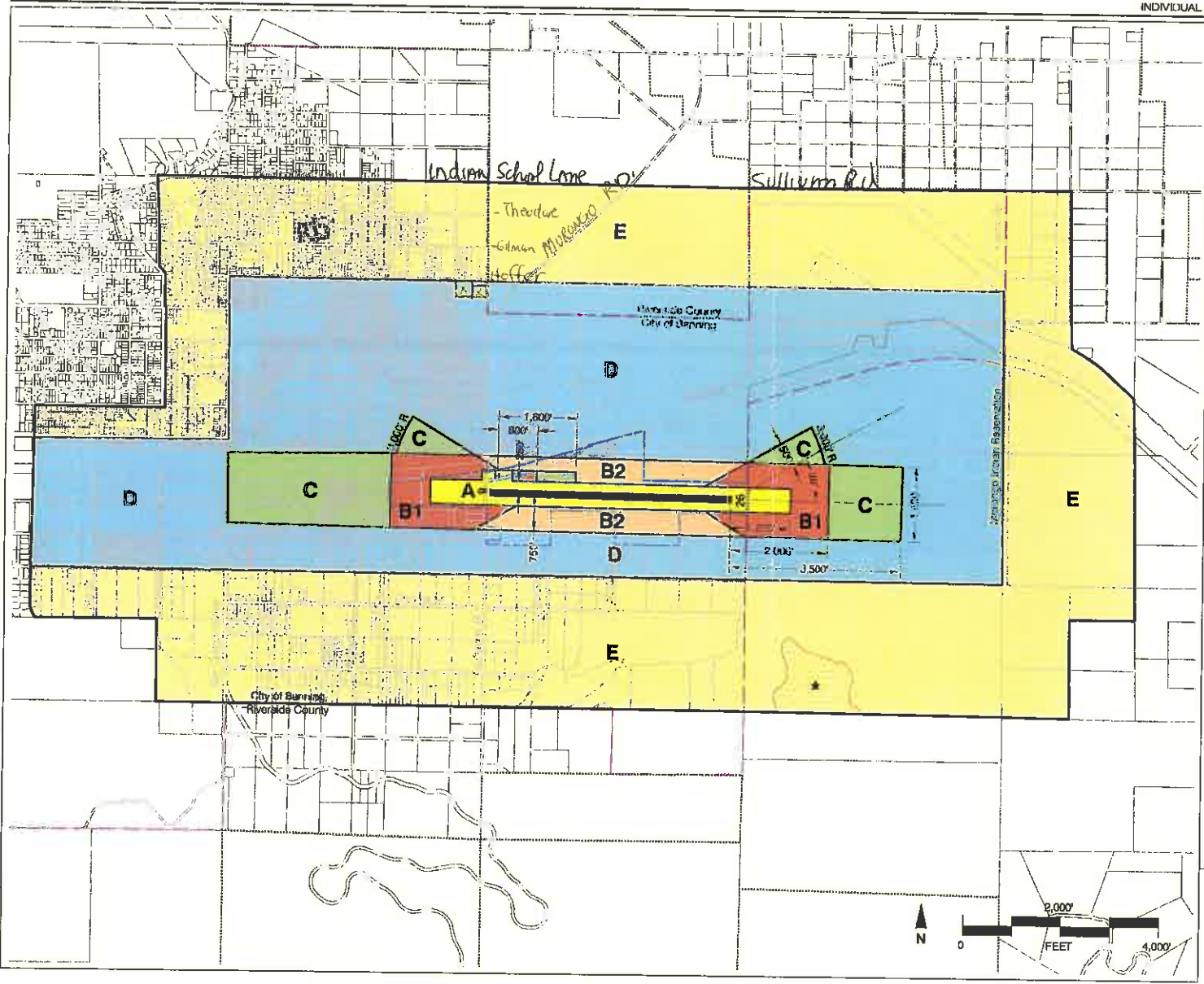
Dimensions measured from runway ends and centerlines.

See Chapter 2, Table 2A for compatibility criteria associated with this map.

Riverside County
 Airport Land Use Commission
**Riverside County
 Airport Land Use Compatibility Plan
 Policy Document**
 (Adapted October 2004)

Map BN-1

Compatibility Map
 Banning Municipal Airport



Legend

- Compatibility Zones**
- Airport Influence Area Boundary
 - Zone A
 - Zone B1
 - Zone B2
 - Zone C
 - Zone D
 - Zone E
 - Height Review Overlay Zone

Boundary Lines

- Airport Property Line
- City Limits
- Morongo Indian Reservation

Note

Dimensions measured from runway ends and centerlines.

See Chapter 2, Table 2A for compatibility criteria associated with this map.

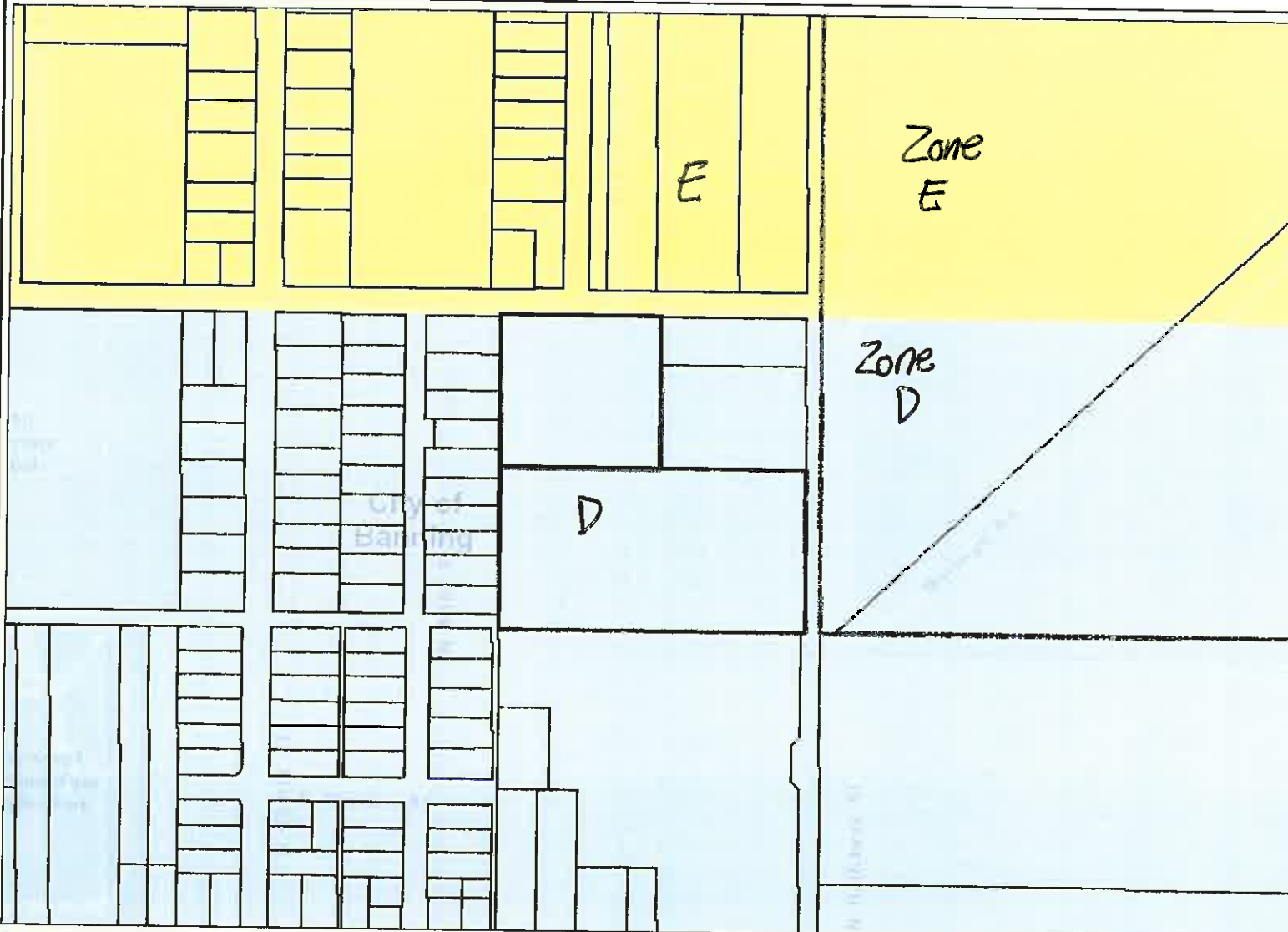
Riverside County
 Airport Land Use Commission
 Riverside County
 Airport Land Use Compatibility Plan
 Policy Document
 (Adopted October 2004)

Map BN-1

Compatibility Map
 Banning Municipal Airport

BNG-compatibility

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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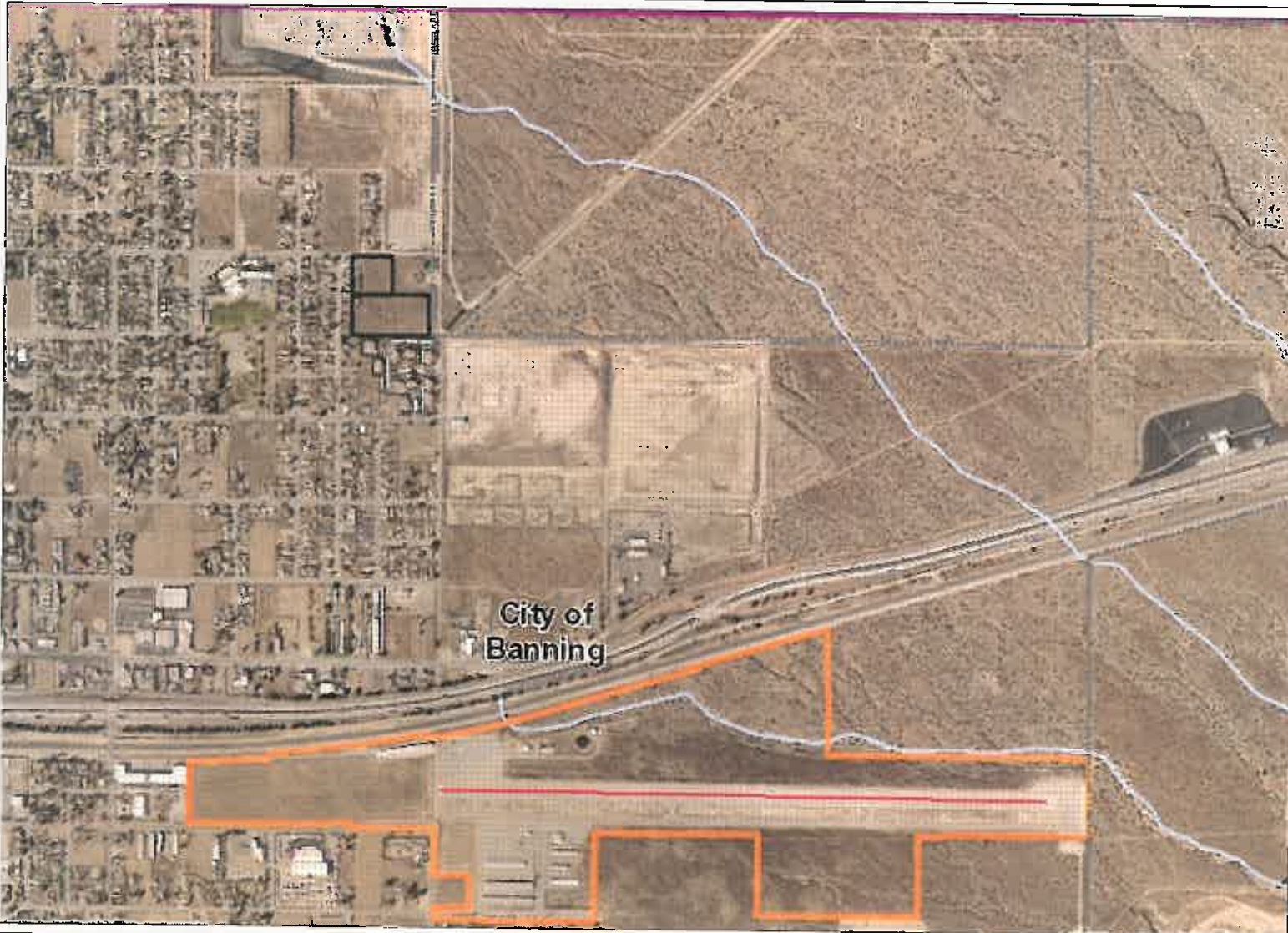
0 376 752 Feet

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Notes

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Blueline Streams
- City Areas
- World Street Map



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Notes

Map My County Map



Legend

-  Parcels
-  Runways
-  Airports
-  Airport Influence Areas
-  Blueline Streams
-  City Areas
-  World Street Map



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0 752 1,505 Feet

REPORT PRINTED ON... 9/19/2018 2:49:02 PM

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Notes

Map My County Map



- Legend**
-  Parcels
 -  Runways
 -  Airports
 -  Airport Influence Areas
 -  Blueline Streams
 -  City Areas
 -  World Street Map



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0 376 752 Feet

REPORT PRINTED ON... 9/19/2018 2:49:37 PM

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Notes

Map My County Map



Legend

-  Parcels
-  Runways
-  Airports
-  Airport Influence Areas
-  Blueline Streams
-  City Areas
-  World Street Map



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0 188 376 Feet

REPORT PRINTED ON... 9/19/2018 2:50:41 PM

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Notes

OWNER/DEVELOPER:

JOHN & DANIA HANNA
18187 KRAMERS AVE
RIVERSIDE, CA 92504
TEL 951 / 453-4997

ENGINEER:

SAKE ENGINEERS INC.
400 S. BANGHAM AVE. STE. 202
CORONA, CA 92679
(951) 279-0041 PH.
(951) 279-2830 FAX

SOIL ENGINEER:

SOIL EXPLORATION COMPANY, INC.
7835 JURUPA AVE. UNIT C
RIVERSIDE, CA 92504
(951) 888-2200 PH.
(951) 888-7100 FAX

ASSESSORS PARCEL NO.:

CS-283-01/2014

TOTAL ACREAGE:

TOTAL ACREAGE (GROSS) 7.98 AC.
DISTURBED AREA 4.18 AC.
IMPERVIOUS AREA 4.08 AC. (178,177.75 S.F.)
PERVIOUS AREA 2.89 AC. (130,227.05 S.F.)

UTILITIES:

ELECTRIC CITY OF BANNING
GAS S.D. GAS COMPANY
WATER CITY OF BANNING
SEWER CITY OF BANNING
TELEPHONE SPECTRUM
CABLE TV SPECTRUM & FRONTIER
SCHOOL DISTRICT BANNING UNITED SCHOOL DISTRICT

TOPOGRAPHY:

ARCHIVED MAPPING CORPORATION
1827 BUSINESS CENTER DR.
SAN BERNARDINO, CA 92401
(909) 889-2420

ZONING AND LAND USE:

EXIST. ZONING LDP
PROP. ZONING HDW
EXIST. LAND USE WRIGHT
PROP. LAND USE APARTMENTS

BASIS OF ELEVATION:

FIELD SURVEY ON 3/17/2018, UN-FOUND
DMS. # 81 TOP OF CURB, SE COR. WILLIAMS
& HANCOCK
ELEV = 5218.61'

BASIS OF BEARING:

THE WEST LINE OF BLOCK 152 BEING NORTH
09°01'51" WEST AS SHOWN ON M.B.18/02

FLOOD ZONE:

FLOOD ZONE = X
COMMUNITY PANEL NO. 0605C-0830
06/28/2008

LEGAL DESCRIPTION:

OF A PORTION OF BLOCK 152 OF BANNING
COLONY LAND, AS SHOWN BY MAP ON FILE IN
BOOK 5 PAGE 186 OF MAPS, RECORDS OF
RIVERSIDE COUNTY.

EARTHWORK:

CUT 04 C12
FILL 04 C12
IMPORT 04 C12

NOTE:

EARTH QUANTITIES SHOWN HERE ARE FOR
ESTIMATING PLAN CHECK FEES ONLY. GRADING
CONTRACTOR IS RESPONSIBLE TO PERFORM
THEIR OWN CALCULATIONS FOR EARTH VOLUME
WITH THE SOILS ENGINEER'S RECOMMENDATION.

LEGEND:

— FLOW LINE
— PROPERTY LINE
— CENTER LINE
— EXISTING WATER LINE
— EXISTING SANITARY SEWER LINE
— EXISTING SLOPE
— EXISTING CONTOUR
— PROPOSED CONTOUR
— RET. WALL
— NEW ASPHALT CONCRETE
— EXISTING CEMENT

C.O. = CLEAN OUT
P. = PROPERTY LINE
S.F. = SQUARE FEET
G.A. = GRADE BREAK
H.P. = HIGH POINT
F.S. = FINISHED SURFACE
S.D. = STORM DRAIN
R.L. = FLOW LINE
T.S. = TOP OF CURB
M.C.L. = MOUNDING CURB
C.L. = CROWN LINE
P.L.P. = PROTECT BY PLACE
T.W. = TOP OF WALL
F. = TOP OF FOOTING
G. = CENTERLINE

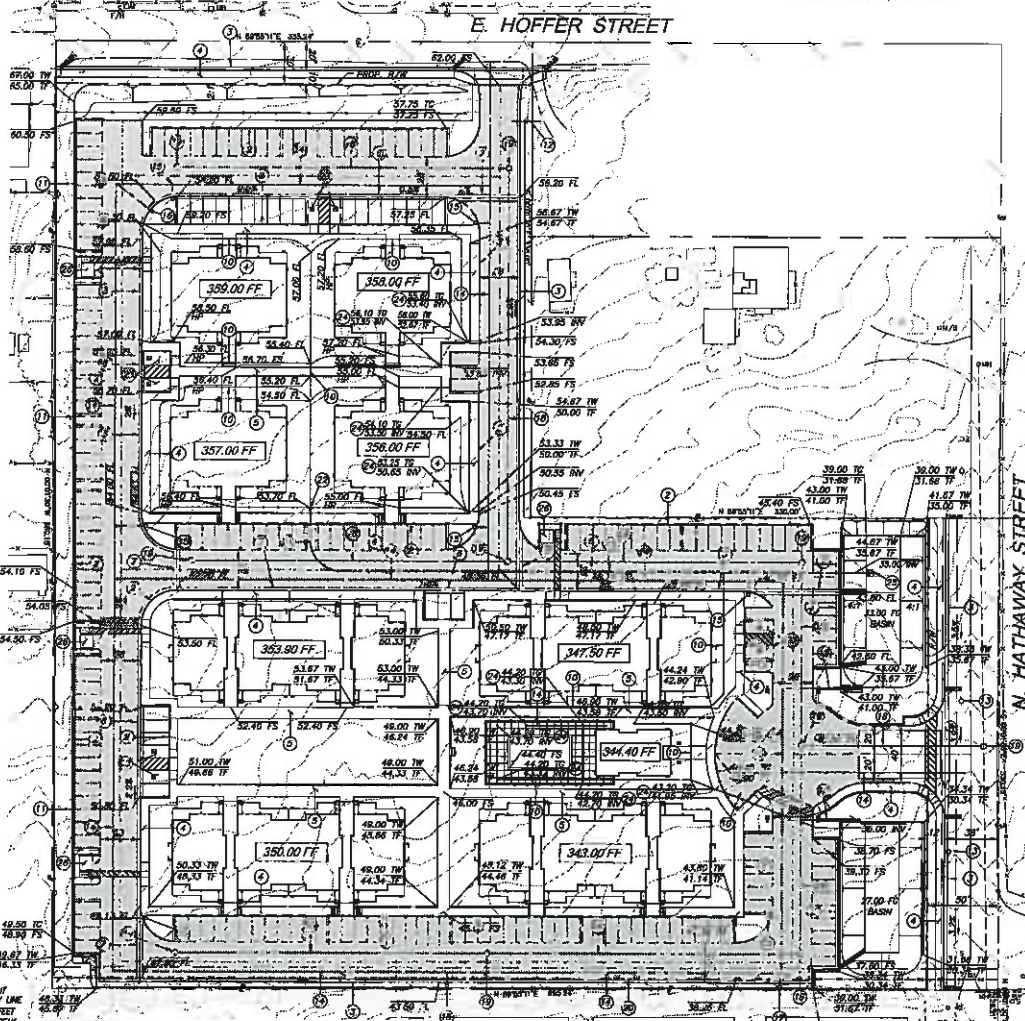
M. = WATER LINE
PROP. = PROPOSED
A.P.N. = ADJACENT PARCELS NUMBER
P.M. = PARCELS MAP
A.C.D. = ADJACENT DRAINAGE
S/A = SET TO SCALE
S/A = SEWER
R.O.W. = RIGHT OF WAY
F.C. = PORTLAND CEMENT CONCRETE
C.C. = CEMENT
C.S. = EXISTING
C. = ASPHALT CONCRETE
C.B.P. = CONSTRUCTION WITHOUT PERMIT

CITY OF BANNING PLOT PLAN

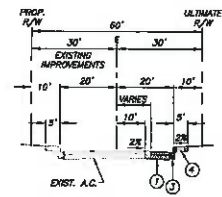
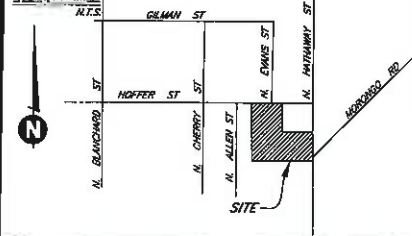
OF A PORTION OF BLOCK 152 OF BANNING COLONY LAND, AS SHOWN BY MAP
ON FILE IN BOOK 5 PAGE 186 OF MAPS, RECORDS OF RIVERSIDE COUNTY.

SAKE ENGINEERS, INC.

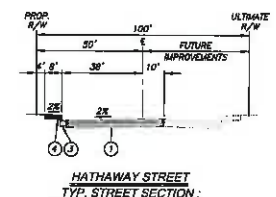
AUGUST, 2018



VICINITY MAP



HOFFER STREET
TYP. STREET SECTION



HATHAWAY STREET
TYP. STREET SECTION

CONSTRUCTION NOTES:

1. CONST. " " AC OVER " " AIR PER SOIL ENG.
2. CONST. 6" CURB
3. CONST. MODIFIED 6" CURB & GUTTER
4. CONST. SIDEWALK
5. CONST. 4" PCC WALKWAY
6. CONST. RIBBON GUTTER W=3"
7. CONST. RIBBON GUTTER W=4"
8. CONST. DRIVEWAY APPROACH
9. CONST. 6" PVC SCH 40 DRAIN PIPE
10. CONST. 4" PVC SCH 40 DRAIN PIPE
11. CONST. RETAINING WALL
12. CONST. GATE PER ARCHITECT PLAN
13. REMOVE EXIST. POWER POLE BY OTHERS
14. CONST. 6" PVC CL C-900 WATERLINE
15. INSTALL FIRE HYDRANT
16. INSTALL 1" AIR VAC
17. INSTALL 1" BLOW OFF
18. CONST. 8" VCP SANITARY SEWERLINE
19. CONST. SEWER MANHOLE
20. RELOCATE EXIST. GASLINE
21. CONST. 18" X 24" CATCH BASIN
22. CONST. VAN ACCESSIBLE PARKING SPACE
23. CONST. 6" DIA AREA DRAIN
24. CONST. 12" DIA RPPAP
25. CONST. COVERED TRASH ENCLOSURE

REVISIONS	DATE	BY

PLOT PLAN CITY OF BANNING

SCALE 1" = 40'

DATE 7/22/2018

DRAWN: TMT

DESIGNED: SA

CHECKED: SA

PLAN OR REF:

SAKE ENGINEERS INC.
ENGINEERING & SURVEYING & LAND DEVELOPMENT
400 S. BANGHAM AVE. STE. 202
CORONA, CALIFORNIA 92679
(951) 279-0041 PH. (951) 279-2830 FAX

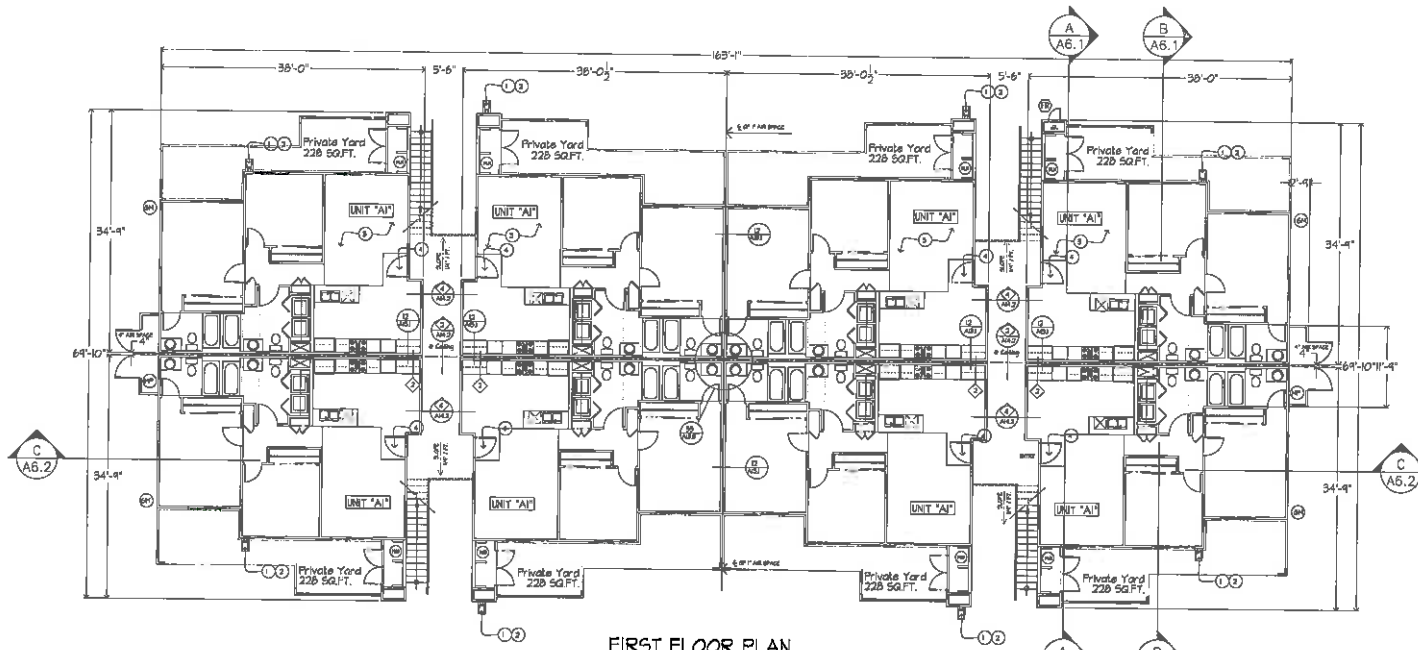
FILE: SERVER\PROJECTS\1801351\DWG\PL01.PLT
PLT DATE: 7/22/2018

J.L. 3153
SHEET
OF 1 SHEETS
DWG. NO.

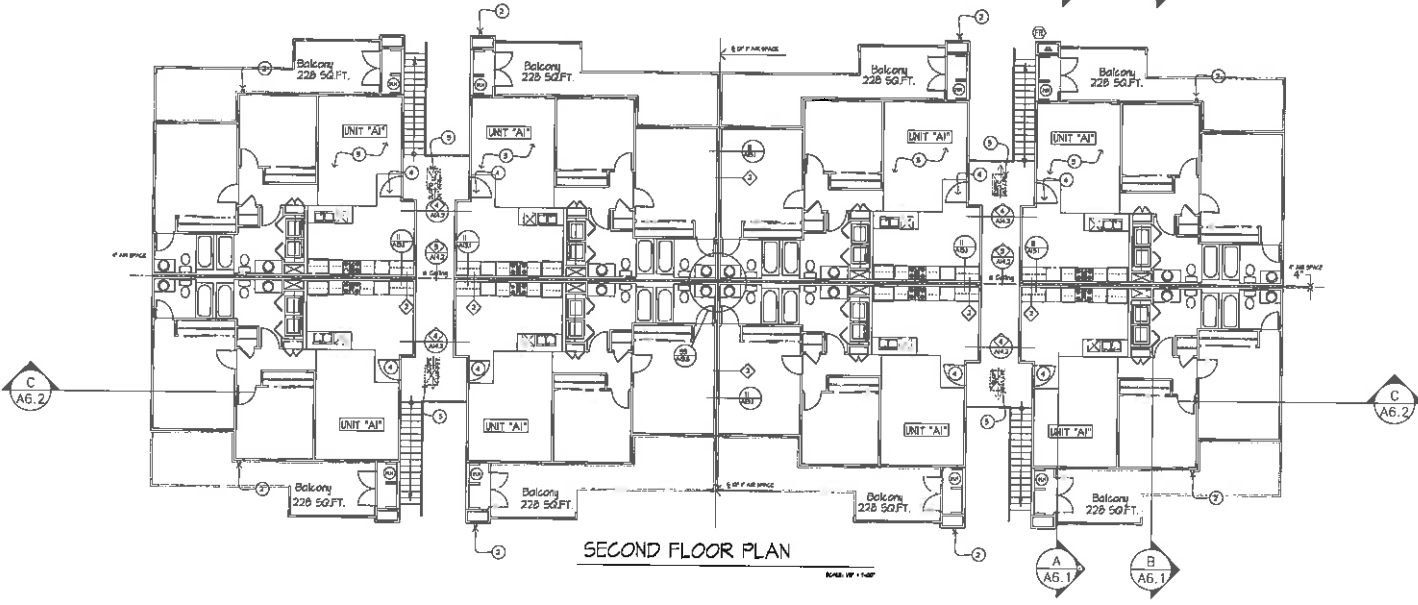
PREPARED UNDER THE DIRECTION OF:

SAM AKBARPOUR P.E. RCE 053038





FIRST FLOOR PLAN



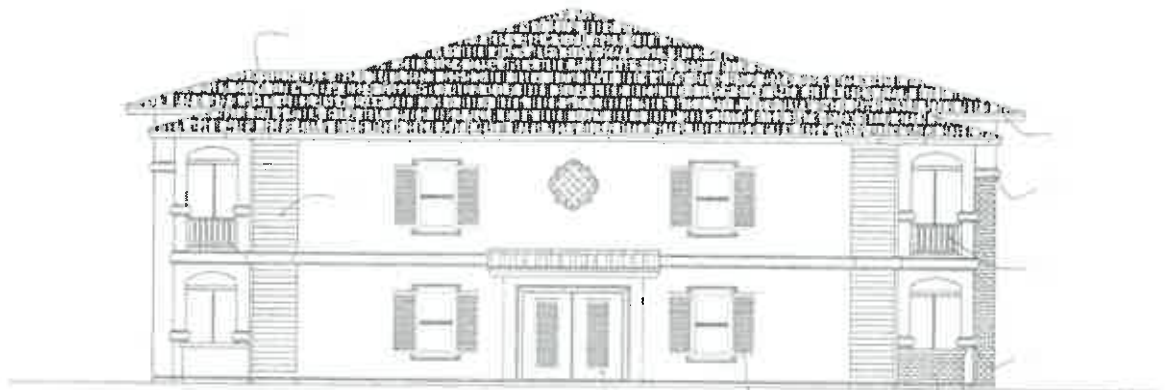
SECOND FLOOR PLAN

32'6" to I.O.F



FRONT ELEVATION

SCALE 1/4" = 1'-0"



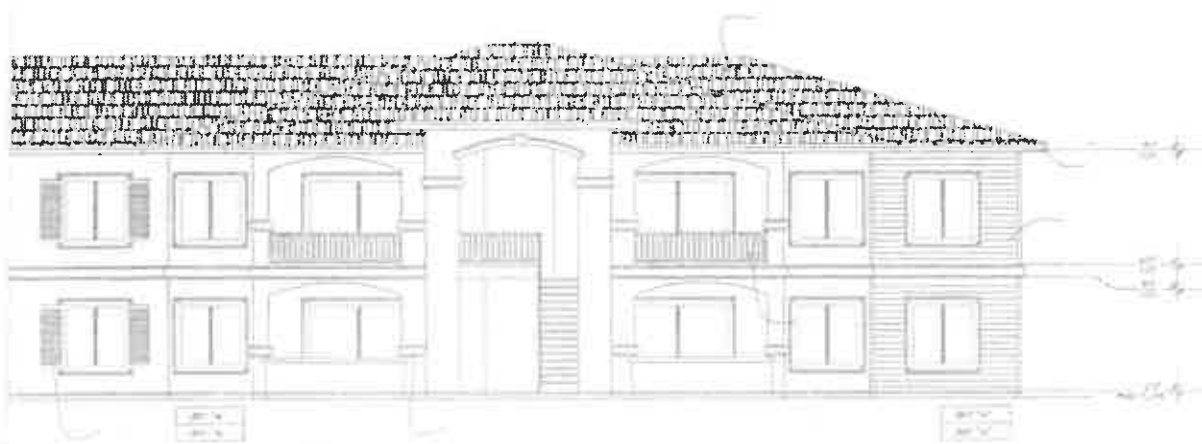
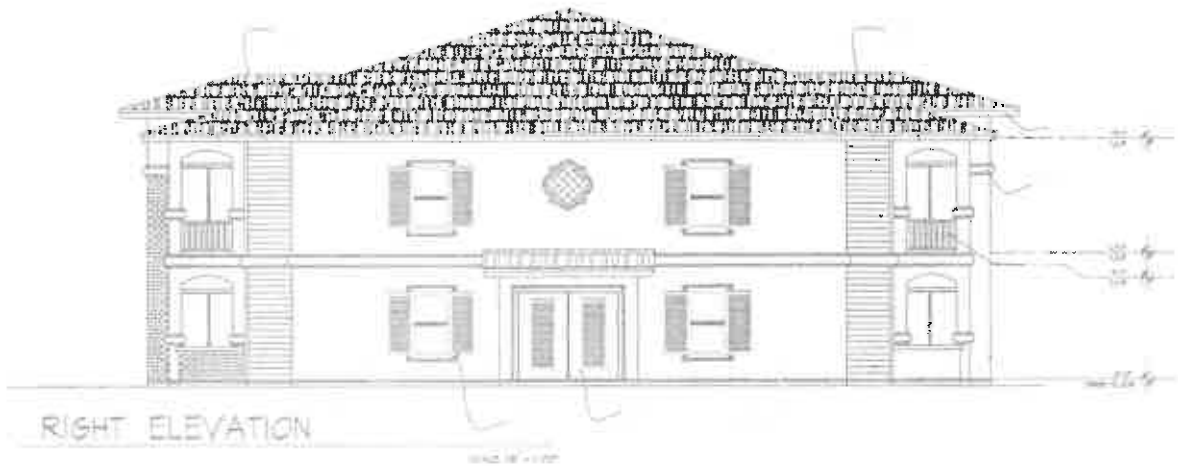
LEFT ELEVATION

SCALE 1/4" = 1'-0"



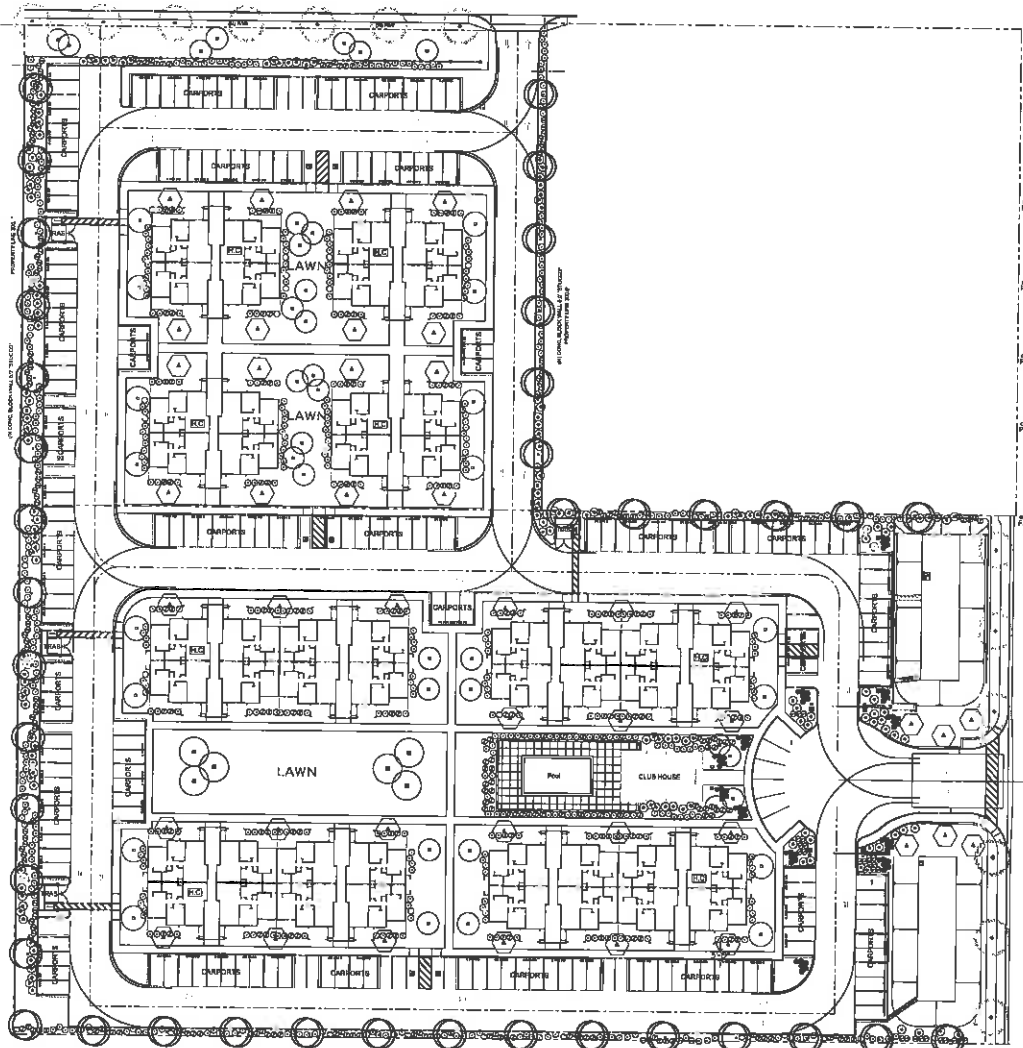
REAR ELEVATION

SCALE 1/4" = 1'-0"



CITY OF BANNING PRELIMINARY LANDSCAPING PLAN EXHIBIT

E. HOFFER STREET



LANDSCAPING LEGENDS

TREES / SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE	QUANTITIES	REMARKS
A	SYRAGLIA	AMERICA STREET BAY	24" DIA	PER PLANT	STREET TREES AT 40' O.C.
B	SPAGNES RENNEZOPHIAH	GREEN PALM	10' DIA		20% TO BE 18" 24" DIA
C	PIRUS KAWAKAWA	ENDORSEAN PEAK	6" DIA		DOUBLE STAKE / HEIGHT 1-3' SPREAD 3-4" DIA 20% TO BE 18" 24" DIA
D	FRAXUS COPALMERA "KAUAI YEWBARD"	FLOWERING PALM	6" DIA		DOUBLE STAKE / HEIGHT 1-3' SPREAD 3-4" DIA 20% TO BE 18" 24" DIA
E	LADONOSTRIMA PALM "TIGER PALM"	RED GOLF HYDRANT	6" DIA		DOUBLE STAKE / HEIGHT 1-3' SPREAD 3-4" DIA 20% TO BE 18" 24" DIA
F	DIACAPTION POPALUM	BOTTLE TREE	6" DIA		DOUBLE STAKE / HEIGHT 1-3' SPREAD 3-4" DIA 20% TO BE 18" 24" DIA

SHRUBS / SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE	QUANTITIES	REMARKS
G	ESCALONIA FRAXEM	ESCALONIA	3" DIA	PER PLANT	FILL AND BUSH
H	HEMEROCALLIS HYBRID	DAYLILY	3" DIA		FILL AND BUSH 1/3 RED 1/3 YELLOW AND 1/3 ORANGE
I	PITOSPORUM TORBA "VARISSA"	VARISSA TORBA	3" DIA		FILL AND BUSH
J	FRAXINOLEPSIS BONGA "LAWA"	BONGA HARTBORN	3" DIA		FILL AND BUSH
K	COTYLS K. BROWNEA?	BROWNEA	3" DIA		FILL AND BUSH
L	SALVIA BRESSEI	ATROMI SAGE	1" DIA		FILL AND BUSH
M	HANDIA DOMESTICA "COMPACTA"	HEAVELY BANDED	1" DIA		FILL AND BUSH

IRRIGATION NOTES

ALL IRRIGATION SYSTEM COMPONENTS SHALL BE INSTALLED AS PER LOCAL CODE. CONTRACTORS SHALL SECURE ALL NECESSARY PERMITS.

CHECK AND VERIFY ALL SITE CONDITIONS, UTILITIES AND SERVICES PRIOR TO TRENCHING. IF DURING CONSTRUCTION IT IS FOUND THAT THE SITE VARIES FROM THE DRAWINGS, NOTIFY THE LANDSCAPE ARCHITECT BEFORE PROCEEDING WITH THE WORK.

WHERE TREES, LIGHT STANDARDS, ETC. ARE AN OBSTRUCTION TO SPRAYS, THEN PIPING AND SPRINKLER HEADS SHALL BE ADJUSTED AND/OR RELOCATED AS NECESSARY TO OBTAIN FULL COVERAGE WITHOUT EXCESSIVE OVERLAPPING. DO NOT EXCEED SPACING AS PER MANUFACTURER RECOMMENDATIONS.

PLANS ARE DIAGRAMMATIC AND APPROXIMATE. ALL PIPING SHALL BE INSTALLED IN PLANTING AREAS EXCEPT WHERE IT IS INFEASIBLE.

CONTRACTOR SHALL FLUSH ALL MAINLINES PRIOR TO INSTALLATION OF VALVES AND ALL LATERAL LINES PRIOR TO INSTALLATION OF HEADS.

SPRINKLER HEADS ARE AVAILABLE IN VARYING DEGREES OF ARCS. CONTRACTOR SHALL PROVIDE AT EACH HEAD THE ARC MOST SUITED TO PROVIDE ADEQUATE COVERAGE AND PREVENT OVERSPRAY ON WALKS, ROADS, BUILDINGS, ETC.

WHERE POSSIBLE, ALL PIPING, P.V.C. ELECTRICAL SLEEVES, ETC., UNDER PAVING SHALL BE INSTALLED PRIOR TO PAVING WORK. NO TEES, ELBS OR OTHER FITS IN PIPING SHALL BE LOCATED UNDER PAVING.

COORDINATE IRRIGATION WORK WITH PLANTING TO AVOID CONFLICTING LOCATIONS BETWEEN PIPING AND PLANT FITS.

ALL QUICK COUPLERS ARE TO BE LOCATED IN SHRUB AREAS, HOWEVER POSSIBLE, AND PLACED 3/4" (6") INCHES FROM OUTSIDE HEADS. IF A QUICK COUPLER IS LOCATED IN A LAWN AREA IT MUST BE PLACED IN A 6" (6") INCH ROAD PLASTIC VALVE BOX.

ALL HEADS, WHEN LOCATED ADJACENT TO CURBS, HEADERS, SIDEWALKS, WALKS, DRIVEWAYS, ETC., ARE TO BE A MIN. OF TWO (2) INCHES TO A MAX. OF FOUR (4) INCHES AWAY FROM THE STRUCTURES.

PLANTING NOTES:

- CONTRACTOR TO CONTACT LANDSCAPING DESIGNER PRIOR TO BEGINNING LANDSCAPE WORK TO REVIEW THE PROJECT UTILITY LOCATIONS AND REVISE PLANS ACCORDINGLY TO FULLY BURDEN UTILITIES FROM VIEW.
- CONTRACTOR RESPONSIBILITY TO PROVIDE PROPER DRAINAGE OF ALL PLANTING AREAS ON SITE AND ALL SUBSURFACE DRAINAGE FOR ALL LOCALIZED PLANTING AREAS.
- IT IS THE CONTRACTOR RESPONSIBILITY TO VISIT AND MARK THE SITE AND TO VERIFY ALL CONDITIONS AND OVERLAPS PRIOR TO PROCEEDING WITH ANY WORK AND TO NOTIFY LANDSCAPE DESIGNER AND OWNER OF ANY OCCURRING.
- CONTRACTOR RESPONSIBILITY TO APPLY "ROAD IP" HEREON ON ALL PLANTING AREAS PRIOR TO PLANTING AS PER MANUFACTURER RECOMMENDATIONS.
- ALL PLANTING AND IRRIGATION INSTALLATIONS SHALL CONFORM TO THE CITY OF TEBULA STANDARDS AND SPECIFICATIONS.
- ALL PLANT QUANTITIES SHOWN ON PLANS TO BE VERIFIED BY THE CONTRACTOR.
- CONTRACTOR SHALL APPLY "TREN STAK" PRE-GENERATED HEREON AT 3' DIA. PER 100' SQ. FT. PLANTING AS PER MANUFACTURER RECOMMENDATIONS ONLY AT NEW SLOPED AREAS.
- CONTRACTOR SHALL REMOVE ALL WEEDS, ROCKS, DEBRIS AND OTHER MATERIALS FROM THE LANDSCAPE AREA PRIOR TO PROCEEDING WITH ANY WORK.
- CONTRACTOR ON SITE TO VERIFY THAT ALL PLANTING SHALL NOT INTERFERE WITH TRAFFIC SITE LINES.
- CONTRACTOR TO INSTALL ROOT BARRIERS WHERE TREES ARE PLANTED WITHIN 5 FEET OF WALKWAY.
- CONTRACTOR SHALL TURN OVER SOIL AND EXISTING VERIFICATION TABLES TO THE CITY INSPECTOR.
- CONTRACTOR TO BE SCHEDULED AND SPOKE TO THAT THEY ARE ADEQUATELY PROTECTED FROM DRIVING OFF FROM SW AND SBY OR FROM ANY OTHER ALLEY.
- CONTRACTOR SHALL APPLY TO ALL WALK PLANTING AREA A 3" LAYER OF 20/40 BROWN GRANITE HOOD MASH OR EQUAL. TERMS VENDOR NO. (P44) 254-0264. AND CONTRACTOR TO PROVIDE TO LANDSCAPE DESIGNER WITH SUPPLY FOR APPROVAL PRIOR TO INSTALLATION.
- CONTRACTOR SHALL CONTACT THE CITY OF TEBULA FOR FINAL INSPECTION WHEN ALL WORK IS COMPLETED.

N. HATHAWAY STREET

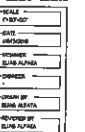


ALPHA DESIGN
14700 HILLY PLACE
INDUSTRIO CA 92568
TEL: 951 300-8541
WWW.ALPHADDESIGN.COM

REVISION BY
DATE



ALPHA DESIGN
14700 HILLY PLACE
INDUSTRIO CA 92568
TEL: 951 300-8541
WWW.ALPHADDESIGN.COM



JOB NO.
18-225

SHEET
A-0



NOTICE OF PUBLIC HEARING RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Thursday from 8:00 a.m. to 4:30 p.m., except Monday, October 8 (Columbus Day), and by prescheduled appointment on Fridays, from 9:00 a.m. to 5:00 p.m.

ATTENTION: ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. The City of Banning will hold hearings on this item and should be contacted on non-ALUC issues.

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside California

DATE OF HEARING: October 11, 2018

TIME OF HEARING: 9:30 A.M.

CASE DESCRIPTION:

ZAP1031BA18 – Diana and John Hanna – City of Banning Case Nos. GPA18-2504 (General Plan Amendment), ZC18-3503 (Zone Change), and DR18-7011 (Design Review). DR18-7011 is a proposal to develop a 96-unit apartment complex with clubhouse on 7.08 – 7.34 acres (Assessor's Parcel Numbers 534-283-011 and 534-283-014) located westerly of Hathaway Street and southerly of Hoffer Street (northerly of an existing apartment complex). GPA18-2504 proposes to amend the designation of the project site on the City's Land Use Map from Low Density Residential to High Density Residential. ZC18-3503 proposes to amend the zoning of the site from Low Density Residential to High Density Residential. (Airport Compatibility Zone D of the Banning Municipal Airport Influence Area)

FURTHER INFORMATION: Contact John Guerin at (951) 955-0982. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Ms. Sonia Pierce of the City of Banning Community Development Department at (951) 922-3152.



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP 1031 BA 18 DATE SUBMITTED: August 29, 2018

APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

Fee: \$2,662

Applicant: John or Diana Hanna Phone Number: 951.453.4997
Mailing Address: 16197 Krameria Ave Email: jhanna83@yahoo.com
Riverside, CA 92504

Representative: N/A (SAME AS ABOVE) Phone Number: _____
Mailing Address: _____ Email: _____

Property Owner: N/A (SAME AS ABOVE) Phone Number: _____
Mailing Address: _____ Email: _____

LOCAL JURISDICTION AGENCY

Local Agency Name: City of Banning Phone Number: 951.922.3152
Staff Contact: Sonia Pierce Email: spierce@cl.banning.ca.us
Mailing Address: 99 E. Ramsey Street Case Type:

Banning, CA 92220
Local Agency Project No: DR18-7011, GPA 18-2504, ZC 18 3503
 General Plan / Specific Plan Amendment
 Zoning Ordinance Amendment
 Subdivision Parcel Map / Tentative Tract
 Use Permit
 Site Plan Review/Plot Plan Design Review
 Other

PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address: Vacant Land at SW side of Hoffer and Hathaway
Banning, CA 92220
Assessor's Parcel No.: 534-283-011 and 014 Gross Parcel Size: 7.34
Subdivision Name: _____ Nearest Airport: _____
Lot Number: _____ Airport: Banning Mun., 1 mi by air

PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe): Vacant land currently zoned low density residential (0-5 du/acre).

Bann
D

Proposed Land Use (describe)	High Density Residential (0-18 du/acre) - we are submitting 13 du/acre	
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)	96
For Other Land Uses (See Appendix C)	Hours of Operation	
	Number of People on Site	Maximum Number
	Method of Calculation	
Height Data	Site Elevation (above mean sea level)	2319.66 ft
	Height of buildings or structures (from the ground)	32'6" ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?	<input type="checkbox"/> Yes
		<input checked="" type="checkbox"/> No
	If yes, describe	

A. NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. REVIEW TIME: Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.

C. SUBMISSION PACKAGE:

- 1. Completed ALUC Application Form
- 1. ALUC fee payment
- 1. Plans Package (24x36 folded) (site plans, floor plans, building elevations, landscaping plans, grading plans, subdivision maps)
- 1. Plans Package (8.5x11) (site plans, floor plans, building elevations, landscaping plans, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
- 1. CD with digital files of the plans (pdf)
- 1. Vicinity Map (8.5x11)
- 1. Detailed project description
- 1. Local jurisdiction project transmittal
- 3. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
- 3. Gummed address labels of all surrounding property owners within a 300 foot radius of the project site (**only required if the project is scheduled for a public hearing Commission meeting**). If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10) with ALUC return address. *

* Projects involving heliports/helicopter landing sites will require additional noticing procedures.

To: Airport Land Use Commission
4080 Lemon Street, 14th Floor
Riverside, CA 92501

August 29, 2018

From: John and Diana Hanna

Re: 96 Units on Hoffer and Hathaway in Banning

Case: DR18-7011

GPACZ

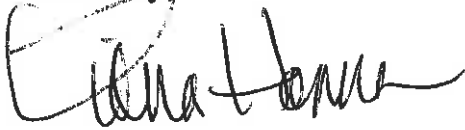
To Whom it may concern;

We are proposing to change APN's 534-283-011 and 534-283-014 from Low-Density Residential to High Density Residential.

The project will consist of 96 2bed/2bath units on the 7.34 acre site. These parcels sit adjacent to another apartment complex.

The project will include private open spaces, 99,000 SF of common open space, 225 parking spaces, a pool, community center and recreational areas.

Sincerely,

A handwritten signature in cursive script, appearing to read "Diana Hanna".

Diana Hanna

16197 Krameria Ave., Riverside, CA 92504 Phone: 951.453.4997

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.4

HEARING DATE: October 11, 2018

CASE NUMBER: ZAP1326MA18 – Stratford Ranch Investors, LLC
(Representative: Jason Keller)

APPROVING JURISDICTION: City of Perris

JURISDICTION CASE NO: 16-05217 (General Plan Amendment), 16-05218 (Zone Change), 16-05216 (Tentative Tract Map No. 36647)

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends that the Commission find the proposed General Plan Amendment and Zone Change CONSISTENT with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, and find the proposed Tentative Tract Map CONSISTENT, subject to the conditions included herein.

PROJECT DESCRIPTION: The applicant proposes to subdivide 24.06 gross acres into 90 single family residential lots, amend the site's General Plan land use designation from Specific Plan (SP) to Single Family Residential R-6,000, and change its zoning from Single Family Residential R-10,000 to Single Family Residential R-6,000.

PROJECT LOCATION: The site is located westerly of Evans Road, northerly of Ramona Expressway, and easterly of Oleander Avenue in the City of Perris, approximately 13,160 feet southeasterly of the southeasterly terminus of Runway 14-32 at March Air Reserve Base.

LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

- a. Airport Influence Area: March Air Reserve Base/Inland Port Airport
- b. Land Use Policy: Airport Compatibility Zones D and E
- c. Noise Levels: below 60 CNEL from aircraft

BACKGROUND:

General Plan Amendment/Zone Change: The project proposes to amend the site's General Plan land use designation from Specific Plan (SP) to Single Family Residential R-6,000, and change its zoning from Single Family Residential R-10,000 to Single Family Residential R-6,000. Both the existing and proposed land use designation and zoning are consistent with Compatibility Zones D and E, where residential density is not restricted.

Residential Density: The project site is split between Airport Compatibility Zones D and E of the March Air Reserve Base/Inland Port Airport. Neither zone limits residential density. The project proposes 90 single family residential lots on 24.06 gross acres, resulting in a density of 3.8 dwelling units per acre.

Prohibited and Discouraged Uses: The only uses prohibited in Airport Compatibility Zones D and E are hazards to flight, and no hazards to flight are proposed by the tract map.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being well outside the 60 CNEL contour relative to aircraft noise. ALUC's objective is that residential interior noise levels from aviation-related sources within this Airport Influence Area not exceed CNEL 40 dB. As standard construction for new homes is presumed to provide adequate sound attenuation where the exterior noise exposure is not more than 20 dB greater than the interior standard, this residential development would not require special measures to mitigate aircraft-generated noise.

Part 77: The elevation of Runway 14-32 at its southeasterly terminus is approximately 1,488 feet above mean sea level (1,488 feet AMSL). The project site is located 13,160 feet from the runway at March Air Reserve Base. At this distance, structures exceeding a top point elevation of 1,619.6 feet AMSL would require notice pursuant to the online Form 7460-1 process. The highest building pad elevation is 1,459 feet AMSL, and the proposed residential building height is 30 feet, for a total maximum elevation of 1,489 feet AMSL. Therefore, review of the proposed project by the FAA Obstruction Evaluation Service is not required for height/elevation reasons.

Open Area: Within the March Air Reserve Base/Inland Port Airport Influence Area, Airport Compatibility Zone D does not require land to be set aside as open areas.

CONDITIONS:

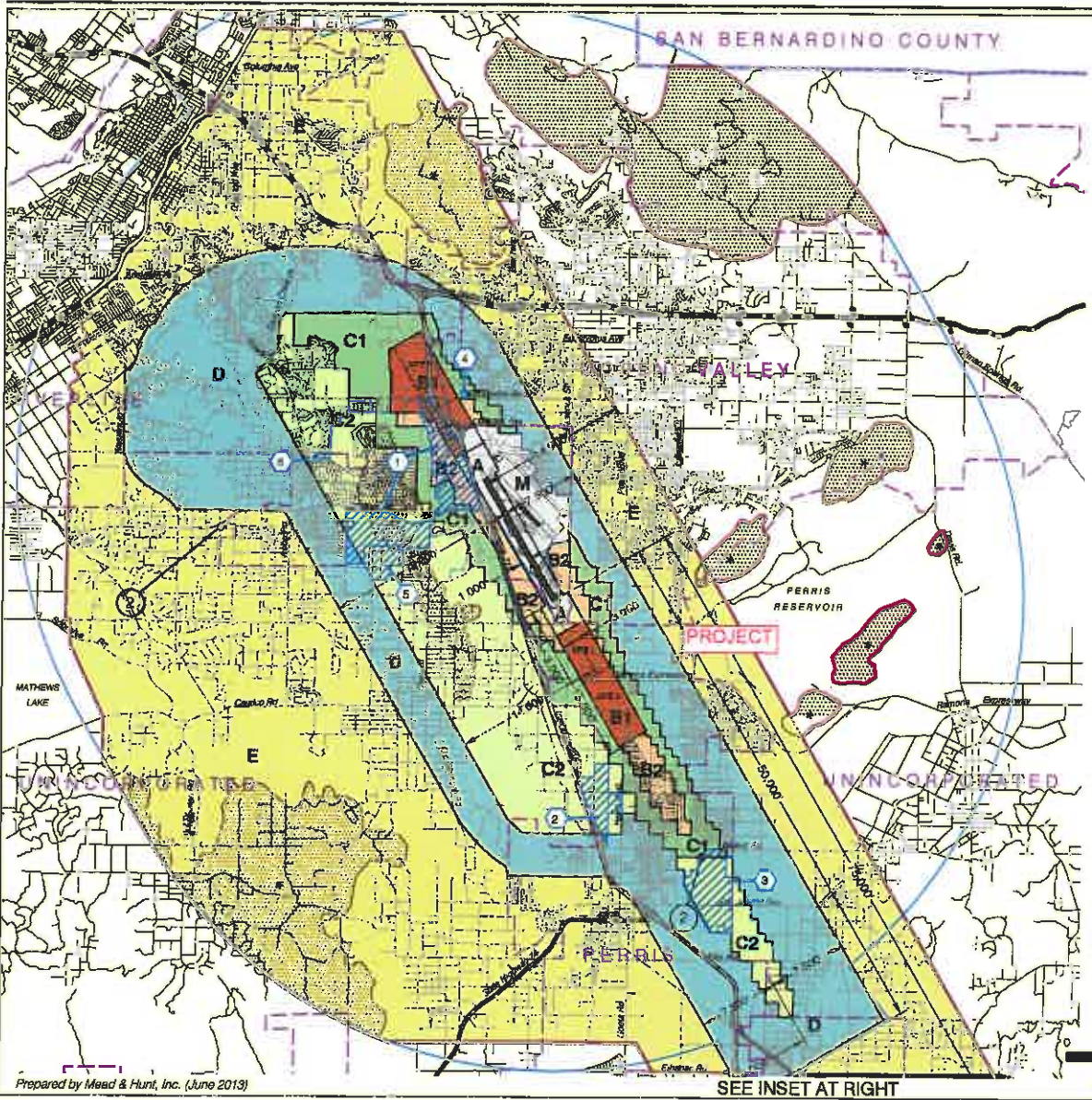
1. Any outdoor lighting installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight

final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.

- (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, artificial marshes, wastewater management facilities, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached disclosure notice shall be provided to all potential purchasers of the proposed lots and to tenants of the homes thereon, and shall be recorded as a deed notice.
 4. The proposed water detention and/or infiltration basins or facilities shall be designed so as to provide for a detention period for the design storm that does not exceed 48 hours and to remain totally dry between rainfalls. Vegetation in and around such facilities that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
 5. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



LEGEND

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

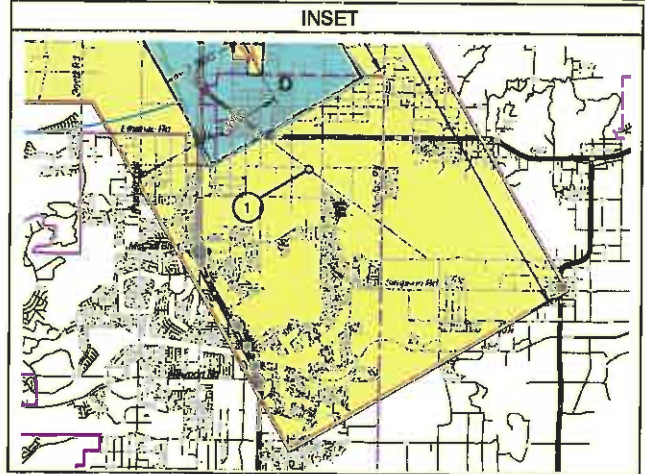
Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

- 1 March JPA: March Business Center/Meridian
- 2 Perris: Harvest Landing
- 3 Perris: Park West
- 4 Moreno Valley: Affordable Housing
- 5 March JPA: Ben Clark Training Center
- 6 Riverside: Ridge Crest Subdivision

1 Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.

2 Point at which departing aircraft typically reach 3,000 feet above runway end.



Riverside County
 Airport Land Use Commission
 March Air Reserve Base / Inland Port Airport
 Land Use Compatibility Plan
 (Adapted November 13, 2014)

Note:
 All dimensions are measured from
 runway ends and centerlines.



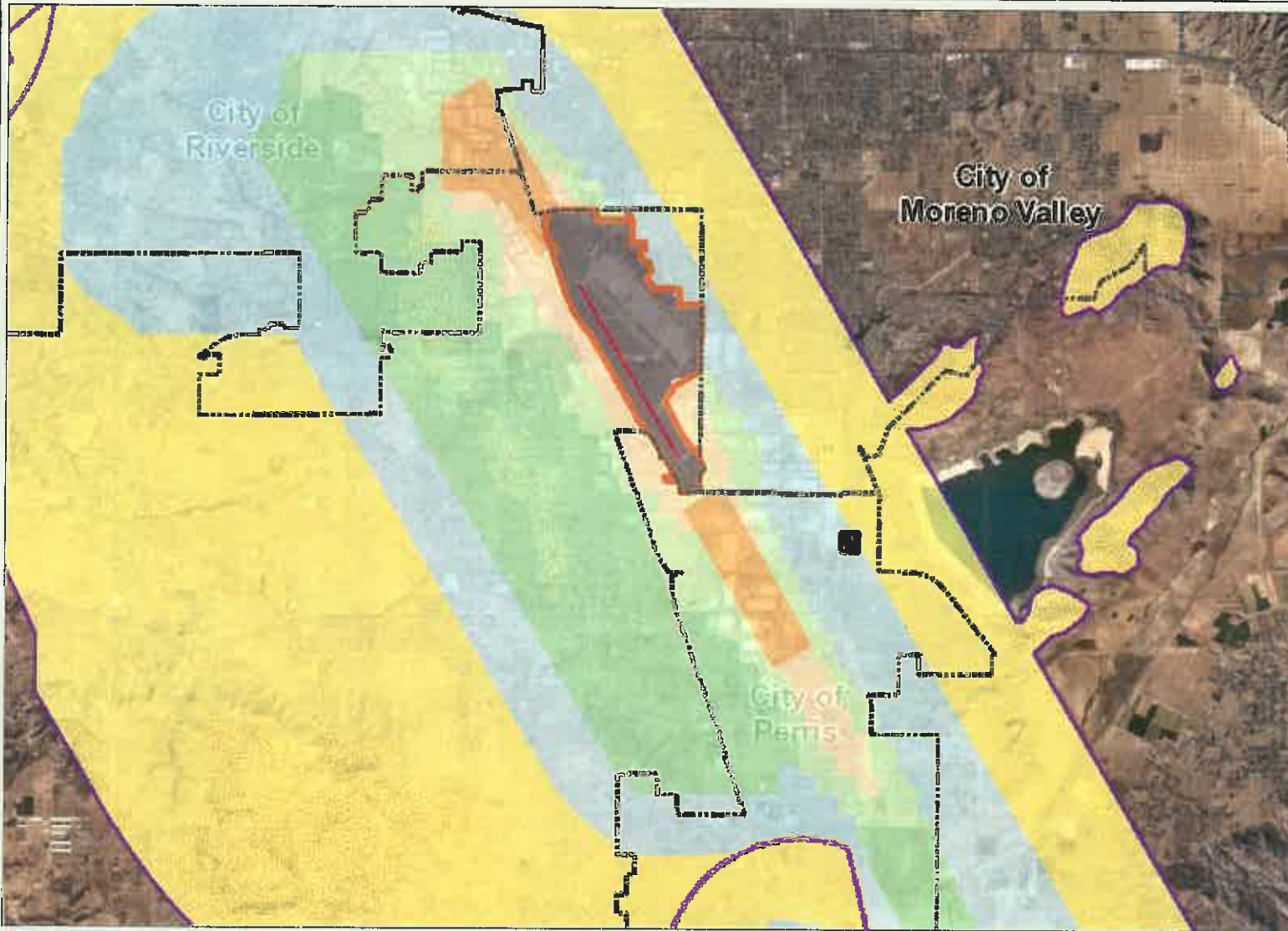
Base map source: County of Riverside 2013

Map MA-1
Compatibility Map
 March Air Reserve Base / Inland Port Airport

SEE INSET AT RIGHT

Prepared by Mead & Hunt, Inc. (June 2013)

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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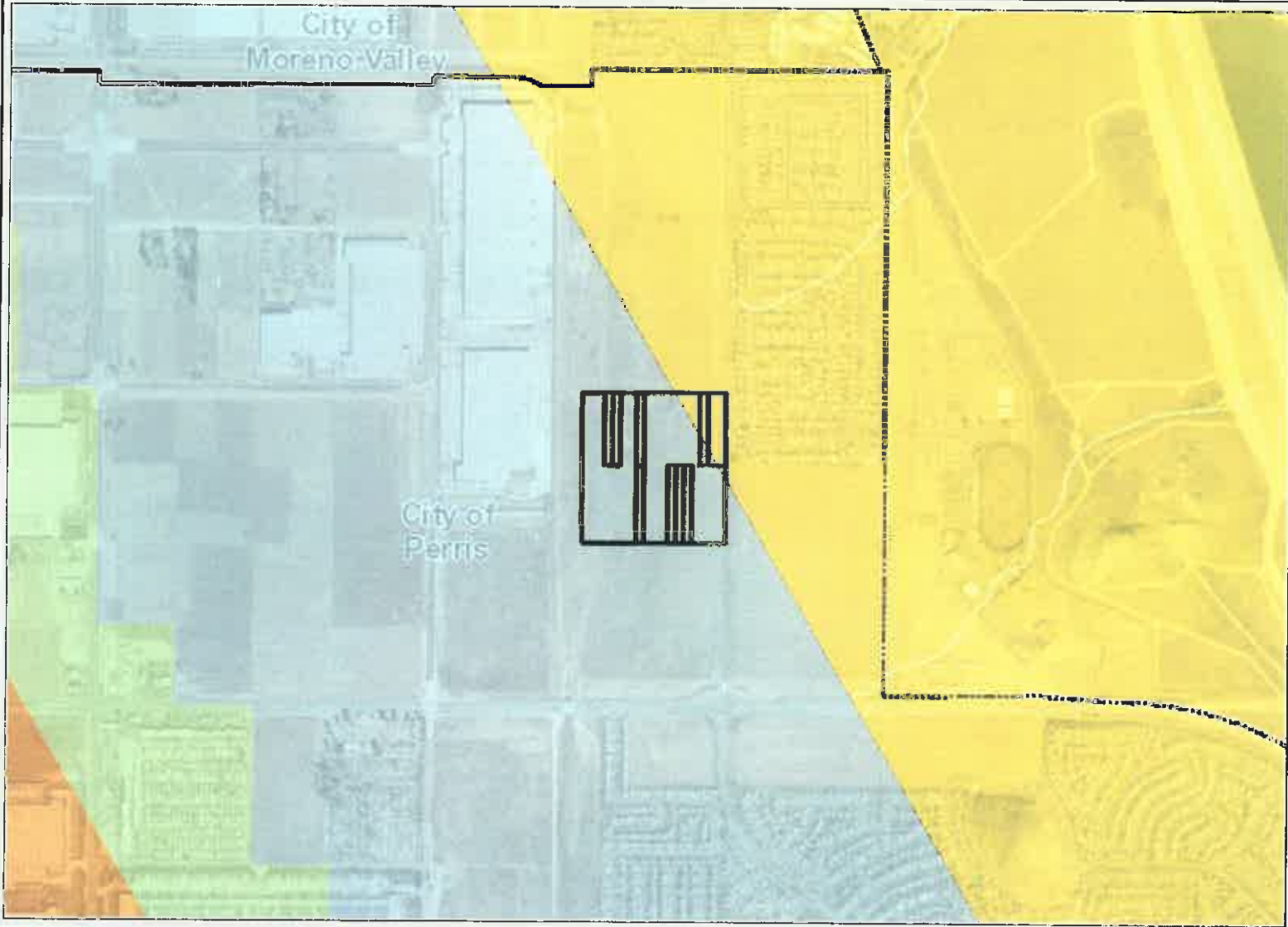


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Notes

Map My County Map



- Legend**
- Runways
 - Airports
 - Airport Influence Areas
 - Airport Compatibility Zones**
 - ▨ OTHER COMPATIBILITY ZONE
 - A
 - A-EXC1
 - B1
 - B1-APZ I
 - B1-APZ I-EXC1
 - B1-APZ II
 - B1-APZ II-EXC1
 - B1-EXC1
 - B2
 - B2-EXC1
 - C
 - C1
 - C1-EXC1
 - C1-EXC3
 - C1-EXC4
 - C1-HIGHT
 - C2
 - C2-EXC1
 - C2-EXC2
 - C2-EXC3
 - C2-EXC5
 - C2-EXC6



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Notes

Map My County Map



Legend

- City Areas
- World Street Map



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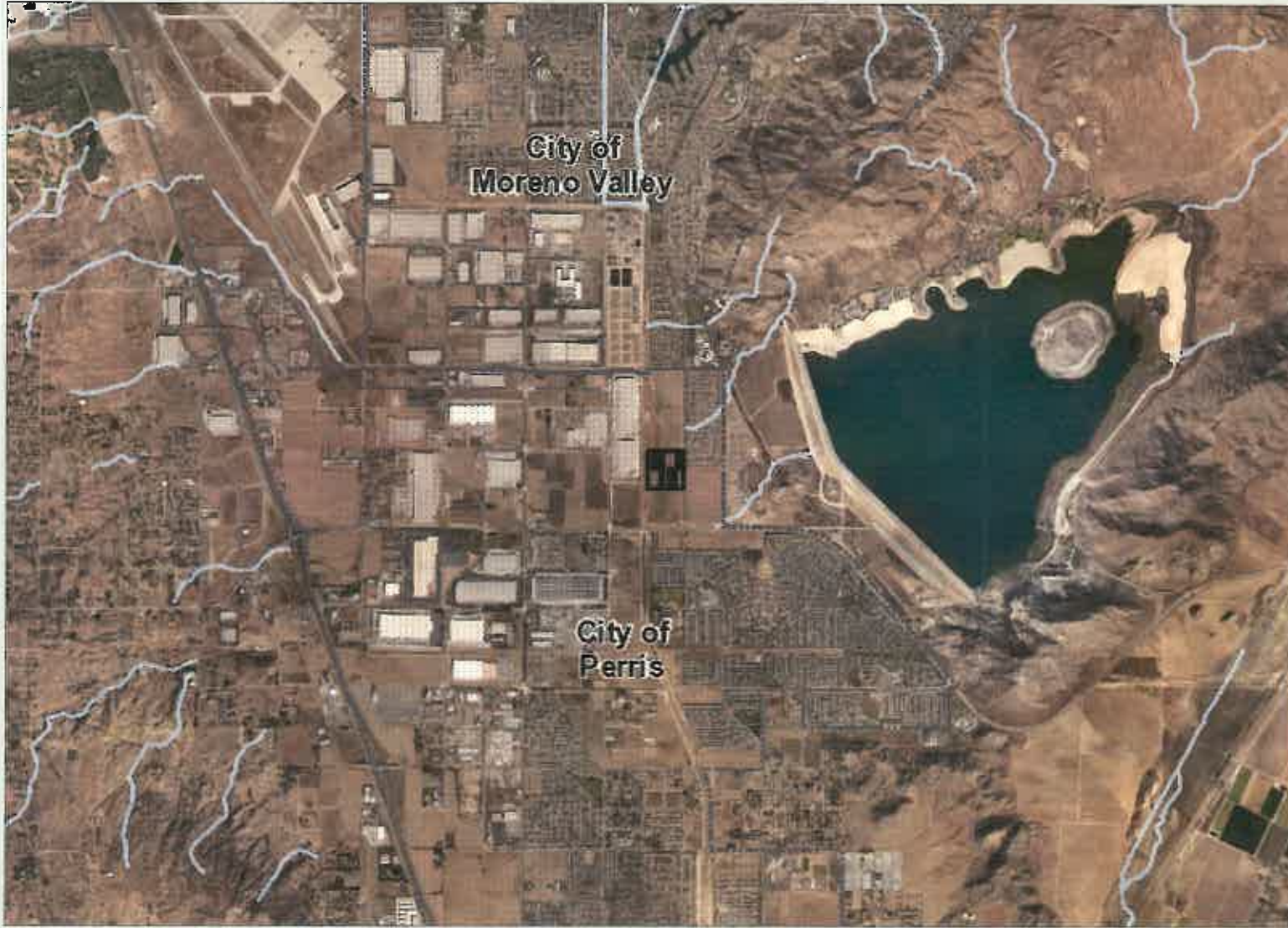


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Notes

Map My County Map



Legend

- Blueline Streams
- ▨ City Areas
- World Street Map



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0 6 12,314 Feet
157

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Notes

Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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Notes

0 1 3,079 Feet
539

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SCALE 1" = 700'



GENERAL PLAN AMENDMENT EXHIBIT GPA 16-05217

CITY OF MORENO VALLEY



EXISTING GENERAL PLAN

-  R-6000
-  SPECIFIC PLAN
-  PUBLIC

LAND USE PER PVCC SPECIFIC PLAN

-  LIGHT INDUSTRIAL
-  PVSD CHANNEL
-  COMMERCIAL

PROPOSED LAND USE

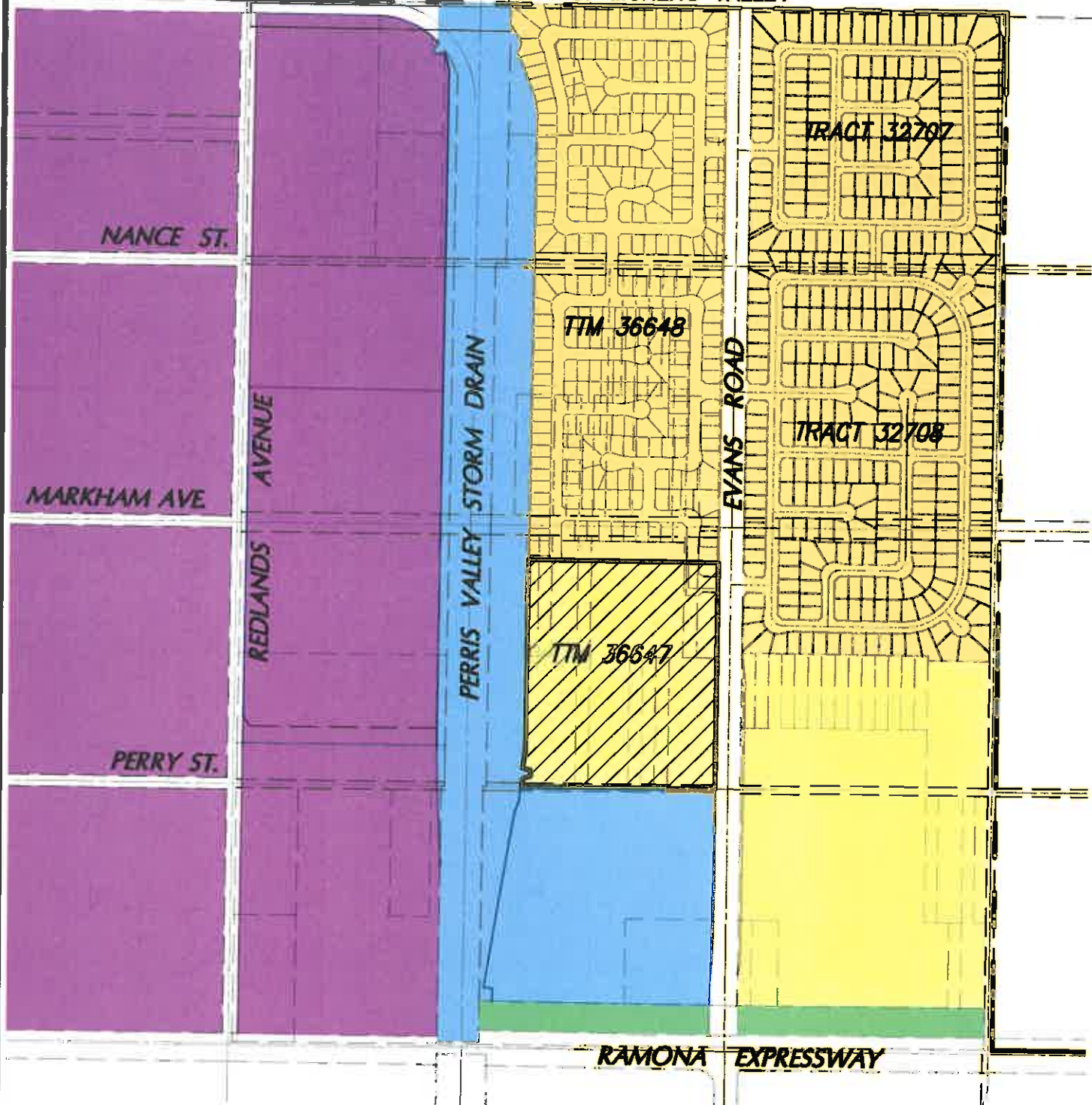
-  R-6000

SCALE 1" = 700'



ZONE CHANGE EXHIBIT ZC 16-05218

CITY OF MORENO VALLEY



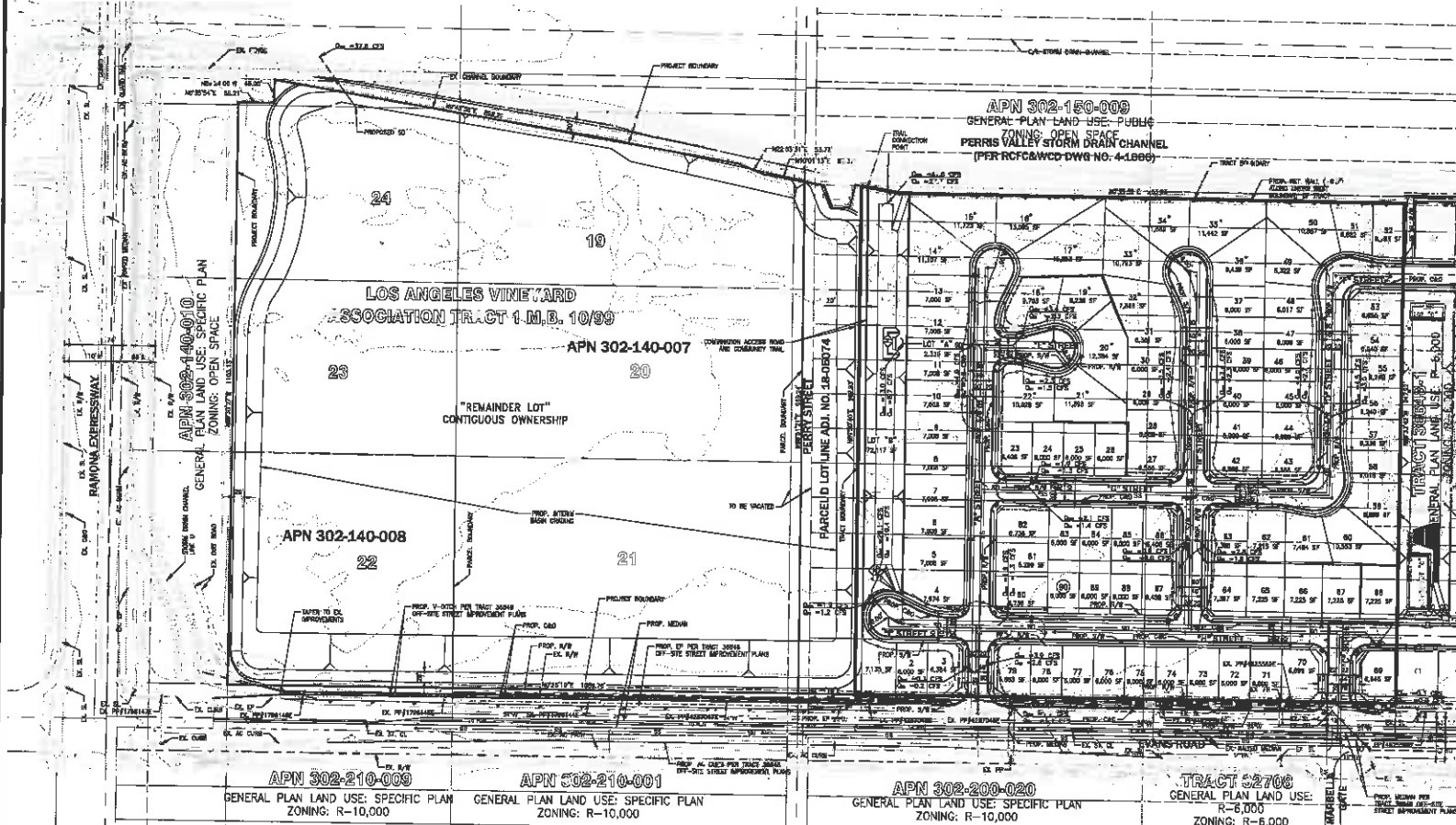
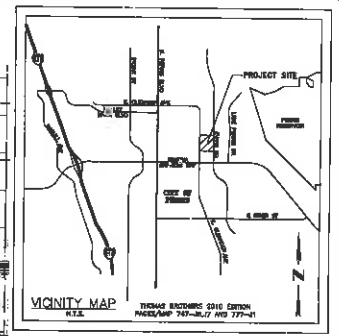
EXISTING ZONING

- | | | | |
|---|--------------------|---|-----------------------|
|  | R-6,000 |  | NC-NEIGHBORHOOD COMM. |
|  | R-10,000 |  | PUBLIC |
|  | PVCC SPECIFIC PLAN |  | OPEN SPACE |

PROPOSED ZONING

- | | |
|---|--------|
|  | R-6000 |
|---|--------|

TENTATIVE TRACT MAP No. 36647



LEGEND

CS	OPEN SPACE
CG	CURB AND GUTTER
C/L	CENTERLINE
EX	EXISTING
GB	GRADE BREAK
HP	HIGH POINT
LF	LOW POINT
FS	FINISH SURFACE
PROP	PROPOSED
P/L	PROPERTY LINE
R/W	RIGHT OF WAY
SC	STORM DRAIN LINE
SF	SQUARE FEET
ST	STREET
SW	SIGNAL
SWR	SEWER LINE
W/L	WATER LINE

LINETYPE LEGEND

---	TRACT BOUNDARY
---	PROPERTY LINE
---	RIGHT OF WAY
---	CENTERLINE
---	PROPOSED EASEMENT LINE
---	CURB
---	SEWER
---	EDGE OF PAVEMENT
---	GUTTER
---	WATER LINE
---	STORM DRAIN LINE
---	RETAINING WALL
---	100% INTERIM WATER SURFACE ELEVATION
---	PROJECT BOUNDARY

- NOTES**
- 2010 THOMAS RANCH MAP BOOK: PAGE 747, GRID J7 & PAGE 777, GRID J1
 - THIS MAP DOES NOT INCLUDE THE ENTIRE CONTIGUOUS OWNERSHIP OF THE OWNER.
 - EASEMENTS OF RECORD ARE SHOWN.
 - THERE ARE NO EXISTING WELLS ON THE PROPERTY.
 - ALL SLOPES ARE 2:1 UNLESS OTHERWISE NOTED.
 - NO SUBSTANTIAL SOFTIC SEWAGE DISPOSAL IS INTENDED.
 - THERE WILL NOT BE ANY FLAMMABLE/COMBUSTIBLE LIQUIDS OR WASTE OIL ABOVE OR BELOW GROUND.
 - LAND IS SUBJECT TO HIGH LOGSLOPE/STABILIZATION.
 - LAND IS NOT WITHIN A SPECIAL STUDIES ZONE.
 - LAND IS SUBJECT TO OVERFLOW, INUNDATION OR FLOOD HAZARD.
 - FLOOD ZONE 4 AREA OF HIGH FLOODING. PER FEMA PANEL OVERLAPPING FLOOD PLAN SHOWN IS THE CURRENT CONDITION. PRIOR TO THE PERRIS VALLEY STORM DRAIN CHANNEL IMPROVEMENT.
 - NO EXISTING STRUCTURES OR DWELLINGS ON SITE.
 - THERE ARE NO OPEN CHANNELS ON SITE.
 - EXISTING FURROW ROADS ARE TO REMAIN IN PLACE.
 - PERRY STREET BETWEEN THE PVED CHANNEL AND EWING ROAD SHALL BE WIDENED UPON RECORDATION OF THIS TRACT MAP.
 - THE BASIS OF BEARINGS FOR THIS SURVEY IS THE CALIFORNIA STATE PLAN COORDINATE SYSTEM, GRID ZONE 4, BASED LOCALLY ON CONTROL STATIONS "LAPP", "PAPP", AND "SPP" (NAD 83/SPR0311).
 - THIS SITE IS BASED ON NAVD 83.

OWNER/DEVELOPER
 STRATFORD RANCH INVESTORS, LLC
 4100 HERRINGTON PLACE DR. SUITE 400
 HERRINGTON BEACH, CA 92660
 PH: (949) 533-6752
 ATTN: JASON HELLER

ENGINEER
 ALBERT A. WEBB ASSOCIATES
 3788 LACKEY STREET
 RIVERSIDE, CA 92506
 ATTN: SHAWN KOWALSKI
 TEL: (951) 686-1170
 FAX: (951) 786-1258

SOILS ENGINEER
 ADVANCED GEOTECHNICAL SOLUTIONS, INC.
 25109 JEFFERSON AVENUE, SUITE 220
 MURKENS, CA 92562
 ATTN: JOFFREY A. CHANLEY
 TEL: (915) 706-1449

TOPOGRAPHY SOURCE
 TOPOGRAPHY FROM BY:
 BLAND AGRIAL SURVEY, INC.
 COMPILED FROM AERIAL PHOTOGRAPHY DATED:
 6/26/2017 AND 10/23/2017

LEGAL DESCRIPTION

TRACT 36647
 PARCELS 2 OF LOT LINE ADJUSTMENT NO. 18-05074
 REC. 1/11/17 INST. 2017-0208312 O.R.

"REMAINDER LOT"
 PARCEL D OF LOT LINE ADJUSTMENT NO. 18-05074
 REC. 1/11/17 INST. 2017-0208312 O.R.

PARCELS 1 (APNs 302-140-000)
 LOTS 18, 19, 20, 21, 22, 24 AND THE EASTERLY 69.7 FEET OF LOT 25 OF
 LOS ANGELES VINEYARD ASSOCIATION TRACT 1, AS SHOWN BY MAP ON FILE
 IN BOOK 10 PAGE 90 OF MAPS, RECORDS OF RIVERSIDE COUNTY, CALIFORNIA;
 EXCEPTING THEREFROM THAT PORTION CONVEYED TO THE RIVERSIDE COUNTY
 FLOOD CONTROL AND WATER CONSERVATION DISTRICT BY DEED RECORDED
 APRIL 8, 1985 IN BOOK 1720 PAGE 226 OF OFFICIAL RECORDS OF RIVERSIDE
 COUNTY, CALIFORNIA;
 ALSO EXCEPTING THEREFROM THAT PORTION CONVEYED TO THE COUNTY OF
 RIVERSIDE BY DEED RECORDED FEBRUARY 11, 1989 AS INSTRUMENT NO.
 11981 OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA;
 ALSO EXCEPTING THEREFROM THAT PORTION CONVEYED TO THE STATE OF
 CALIFORNIA BY DEED RECORDED NOVEMBER 18, 1983 AS INSTRUMENT NO.
 115284 OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA;
 ALSO EXCEPTING THEREFROM THOSE PORTIONS OF LOTS 18, 19, 24 AND 25
 CONVEYED TO THE RIVERSIDE COUNTY FLOOD CONTROL AND WATER
 CONSERVATION DISTRICT BY DEED RECORDED MAY 24, 2017 AS INSTRUMENT
 NO. 2017-0208312 OF OFFICIAL RECORDS OF
 RIVERSIDE COUNTY, CALIFORNIA;
 ALSO EXCEPTING THEREFROM THOSE PORTIONS OF LOT 18 CONVEYED TO THE
 RIVERSIDE COUNTY FLOOD CONTROL AND WATER CONSERVATION DISTRICT BY

DEED RECORDED MAY 24, 2017 AS INSTRUMENT NO. 2017-0208316 OF
 OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA;
 PARCEL 2 (APNs 302-140-000)
 PARCELS 22 OF LOS ANGELES VINEYARD ASSOCIATION TRACT 1, AS SHOWN BY
 MAP ON FILE IN BOOK 10 PAGE 90 OF MAPS, RECORDS OF RIVERSIDE
 COUNTY, CALIFORNIA;
 EXCEPTING THEREFROM THAT PORTION CONVEYED TO THE COUNTY OF
 RIVERSIDE BY DEED RECORDED SEPTEMBER 19, 1968 IN BOOK 2335 PAGE
 23 OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA;
 ALSO EXCEPTING THEREFROM THAT PORTION CONVEYED TO THE STATE OF
 CALIFORNIA BY DEED RECORDED JULY 14, 1970 AS INSTRUMENT NO. 67199
 OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA;
 PARCEL 3 (APNs 302-150-000-C, 302-150-013-A AND
 302-150-013-B)
 PARCELS E OF LOT 4, PARCELS E OF LOT 5, PARCELS A, C, D AND E OF LOT
 6, PARCELS A, B, C, D AND E OF LOT 7, PARCELS D, E AND F OF LOT 8,
 PARCELS A, B, C, D AND E OF LOT 9, PARCELS C, D AND E OF LOT 10
 AND PARCEL E OF LOT 11, ALL IN UNIT 1 OF LA VINA LAND COMPANY
 TRACT, UNITS 1 AND 2 AS SHOWN BY MAP ON FILE IN BOOK 14 PAGES 19
 AND 20 OF MAPS, RECORDS OF RIVERSIDE COUNTY, CALIFORNIA;
 EXCEPTING FROM PARCEL E OF LOT 4 AND PARCEL E OF LOT 10 THOSE
 PORTIONS DESCRIBED IN PARCEL 4 AS CONVEYED TO THE RIVERSIDE COUNTY
 FLOOD CONTROL AND WATER CONSERVATION DISTRICT BY DEED RECORDED
 APRIL 8, 1985 IN BOOK 1720 PAGE 226 OF OFFICIAL RECORDS OF RIVERSIDE
 COUNTY, CALIFORNIA;
 ALSO EXCEPTING THEREFROM THOSE PORTIONS OF PARCEL E OF LOT 4,
 PARCEL C OF LOT 10, PARCEL E OF LOT 11 CONVEYED TO THE RIVERSIDE
 COUNTY FLOOD CONTROL AND WATER CONSERVATION DISTRICT BY DEED
 RECORDED MAY 24, 2017 AS INSTRUMENT NO. 20170208312 OF OFFICIAL

RECORDS OF RIVERSIDE COUNTY, CALIFORNIA;
 ALSO EXCEPTING THEREFROM THOSE PORTIONS OF C OF LOT 10 CONVEYED TO
 THE RIVERSIDE COUNTY FLOOD CONTROL AND WATER CONSERVATION DISTRICT
 BY DEED RECORDED MAY 24, 2017 AS INSTRUMENT NO. 2017-0208312 OF
 OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA;
 PARCELS 4 (APNs 302-150-013-B AND 302-150-013-C)
 OF MAPS, RECORDS OF RIVERSIDE COUNTY, CALIFORNIA;
 UNITS 1 AND 2 AS SHOWN BY MAP ON FILE IN BOOK 14 PAGES 19 AND 20
 OF MAPS, RECORDS OF RIVERSIDE COUNTY, CALIFORNIA;
 PARCELS 4 (APNs 302-150-013-B AND 302-150-013-C)
 PARCEL C, LOT E, UNIT NO. 1, OF LA VINA LAND TRACT AS SHOWN BY MAP
 ON FILE IN BOOK 14 PAGES 19 AND 20 OF MAPS, RECORDS OF RIVERSIDE
 COUNTY, CALIFORNIA;

ASSESSOR'S PARCEL NO.
 ITN 36647
 302-150-010, 302-150-011, 302-150-012, 302-150-016,
 302-150-018, 302-150-019, 302-150-020, 302-150-013,
 302-150-014, 302-150-015, 302-150-016, 302-150-017

UNIMPAVED LOT
 302-140-007 302-140-008
 AND PORTIONS OF APNS: 302-150-009, 302-150-013,
 302-150-014, 302-150-015, 302-150-016, 302-150-017

NOTE
 ALL HOMES ARE SUBJECT TO CITY OF PERRIS AUTOMATIC FIRE
 SPRINKLER REQUIREMENTS. PER CITY OF PERRIS FIRE SECTION
 15-3-1 REQUIREMENTS, AND THE SPRINKLER SYSTEM SHALL
 INCLUDE FULL PROTECTION OF THE ATTIC SPACE(S).

UTILITIES
 WATER: EASTERN MUNICIPAL WATER DISTRICT
 EASTERN MUNICIPAL WATER DISTRICT
 ELECTRIC: SOUTHERN CALIFORNIA Edison COMPANY
 GAS: SOUTHERN CALIFORNIA GAS COMPANY
 TELEPHONE: SBC
 TELEVISION: INDIVIDUAL RECEPTION/CABLE

SCHOOL DISTRICT
 WAL WOOD UNIFIED SCHOOL DISTRICT

EARTH-WORK
 AMF: 223,410 CY
 RLL: 147,670 CY

LAND USE
 EXISTING LAND USE, SPECIFIC PLAN
 PROPOSED LAND USE: R-8,000
 ZONING: R-10,000
 PROPOSED ZONING: R-8,000

ACREAGE
 TOTAL 36647
 24.12 AC. GROSS
 22.82 AC. NET
 QUANTITY: 40 D/U AC

REMAINDER LOT
 30.70 AC. GROSS
 28.94 AC. NET

GRAND TOTAL
 54.82 AC. GROSS
 51.54 AC. NET

1" = 100'
 0 100 200 300
 CITY CASE No. 16-05216

TENTATIVE MAP
TRACT 36647
COMPREHENSIVE SITE PLAN

SCALE: 1" = 100'
 DATE: 6/23/2017
 DESIGNED: CCA
 CHECKED BY: A
 PLOTTED: 8/1

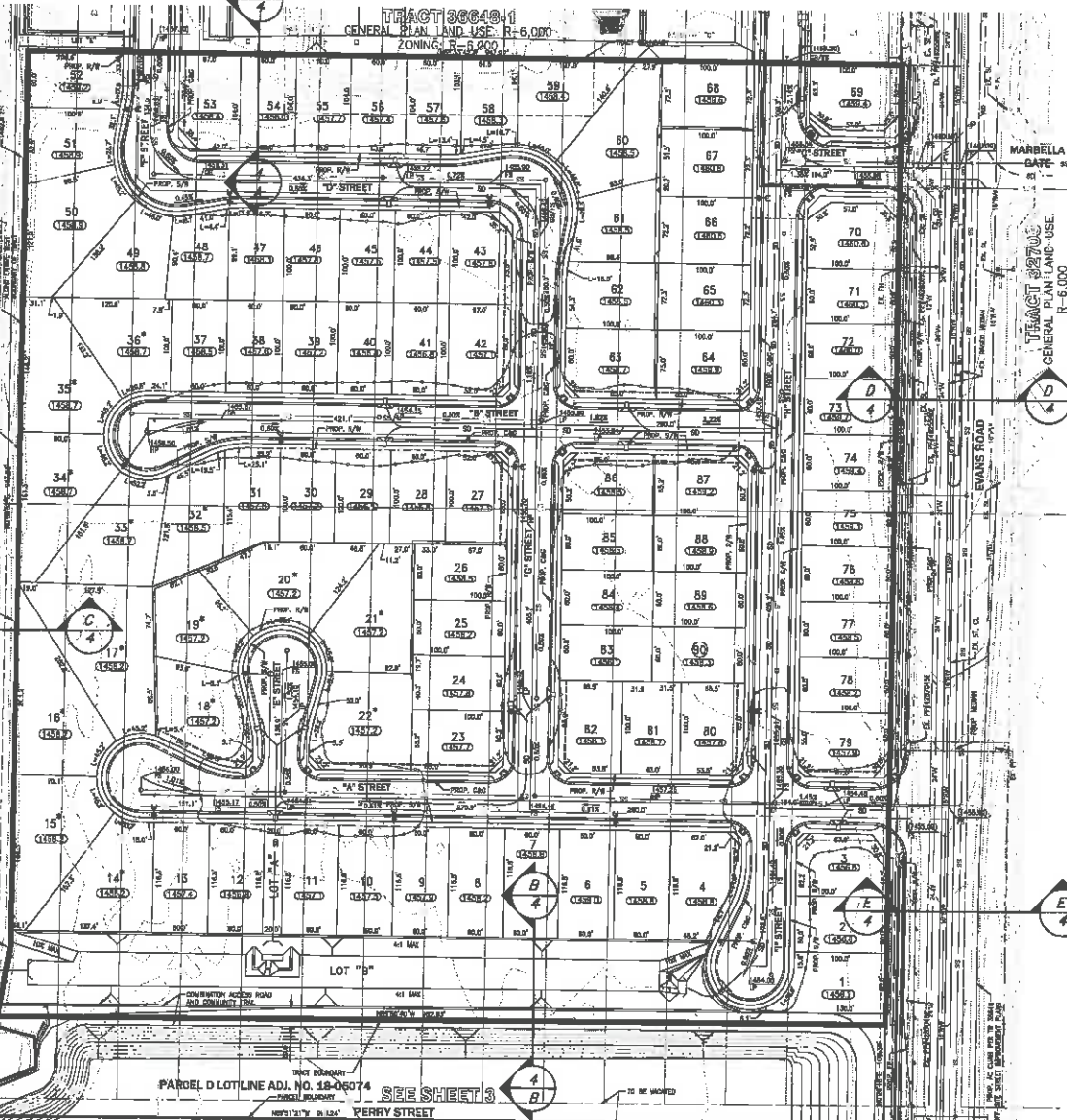
PREPARED FOR: STRATFORD RANCH INVESTORS, LLC
 4100 HERRINGTON PLACE, SUITE 400
 HERRINGTON BEACH, CA 92660
 TEL: (949) 533-6752

PROJECT NO.: 16-05216
 SHEET: 1
 OF 4 SHEETS
 DWG. NO.

TENTATIVE TRACT MAP No. 36647 CONCEPTUAL GRADING

APN 302-150-009
GENERAL PLAN LAND USE PUBLIC
ZONING: OPEN SPACE

PERRIS VALLEY STORM DRAIN CHANNEL
(PER ROPCAMCD DWG NO. 4-1086)



LOT #	LOT SIZE (SF)	USABLE AREA	LOT WIDTH	LOT DEPTH	STREET FRONTAGE	LOT #	LOT SIZE (SF)	USABLE AREA	LOT WIDTH	LOT DEPTH	STREET FRONTAGE
1	7135	7135	65.4'	100.0'	72.5'	47	8008	6006	60.0'	60.0'	60.0'
2	6000	6000	60.0'	100.0'	60.0'	48	6917	6017	62.7'	60.2'	63.4'
3	8384	8672	87.2'	100.0'	52.2'	49	6322	6322	60.4'	60.2'	45.2'
4	7874	7185	73.9'	116.8'	62.0'	50	10667	10667	60.3'	66.3'	45.2'
5	7008	7008	60.0'	116.8'	60.0'	51	6952	6952	64.8'	64.3'	60.3'
6	7008	7008	60.0'	116.8'	60.0'	52	8193	6978	60.0'	100.0'	60.3'
7	7008	7008	60.0'	116.8'	60.0'	53	8835	6447	65.0'	104.0'	40.3'
8	7008	7008	60.0'	116.8'	60.0'	54	8240	8240	60.0'	104.0'	60.0'
9	7008	7008	60.0'	116.8'	60.0'	55	8240	8240	60.0'	104.0'	60.0'
10	7008	7008	60.0'	116.8'	60.0'	56	8240	8240	60.0'	104.0'	60.0'
11	7008	7008	60.0'	116.8'	60.0'	57	6236	6236	60.0'	103.4'	60.0'
12	7008	7008	60.0'	116.8'	60.0'	58	8018	6018	62.0'	64.1'	62.7'
13	7008	7008	60.0'	116.8'	60.0'	59	8898	6899	60.0'	64.1'	44.6'
14	11107	11107	61.7'	116.8'	45.7'	60	10533	10162	59.6'	65.3'	44.6'
15	11723	11723	66.3'	80.1'	45.2'	61	7464	7086	73.3'	66.3'	71.2'
16	13065	13065	70.8'	80.1'	46.2'	62	7215	6933	72.3'	66.4'	72.3'
17	15853	15853	80.0'	161.1'	45.2'	63	7385	6601	75.0'	100.0'	60.0'
18	8703	8008	61.1'	80.0'	50.0'	64	7387	6768	75.0'	100.0'	60.0'
19	8238	7893	60.5'	90.6'	45.7'	65	7225	7225	72.3'	100.0'	72.3'
20	12364	12188	108.7'	95.2'	65.1'	66	7225	7225	72.3'	100.0'	72.3'
21	11309	11038	82.3'	82.3'	45.2'	67	7225	7225	72.3'	100.0'	72.3'
22	10225	10229	106.4'	82.3'	61.5'	68	7225	7006	72.3'	100.0'	72.3'
23	9408	6788	55.2'	100.0'	50.2'	69	8845	6945	70.5'	100.0'	46.3'
24	6000	6000	60.0'	100.0'	60.0'	70	8956	6959	70.5'	100.0'	46.3'
25	6000	6000	60.0'	100.0'	60.0'	71	8000	6000	60.0'	100.0'	60.0'
26	8000	5881	60.0'	100.0'	60.0'	72	8000	6000	60.0'	100.0'	60.0'
27	8588	5782	60.0'	100.0'	60.0'	73	8000	6000	60.0'	100.0'	60.0'
28	9000	5887	60.0'	100.0'	60.0'	74	8000	6000	60.0'	100.0'	60.0'
29	6000	6000	60.0'	100.0'	60.0'	75	8000	6000	60.0'	100.0'	60.0'
30	6000	6000	60.0'	100.0'	60.0'	76	8000	6000	60.0'	100.0'	60.0'
31	6361	6361	60.0'	100.0'	60.2'	77	6000	6000	60.0'	100.0'	60.0'
32	7866	7866	62.2'	115.1'	66.0'	78	6000	6000	60.0'	100.0'	60.0'
33	10793	10793	68.0'	121.5'	66.7'	79	6663	6161	70.0'	100.0'	55.0'
34	11568	11568	66.4'	90.0'	45.2'	80	6738	6206	70.0'	100.0'	55.0'
35	11442	11442	66.2'	60.0'	45.3'	81	6239	6002	60.0'	100.0'	60.0'
36	9436	9436	74.1'	100.0'	55.8'	82	6736	6006	70.0'	100.0'	55.0'
37	6000	6000	60.0'	100.0'	60.0'	83	6000	6000	60.0'	100.0'	60.0'
38	8000	6000	60.0'	100.0'	60.0'	84	6000	6000	60.0'	100.0'	60.0'
39	8000	6000	60.0'	100.0'	60.0'	85	6000	6000	60.0'	100.0'	60.0'
40	6000	6000	60.0'	100.0'	60.0'	86	8408	6842	65.1'	100.0'	50.1'
41	6000	6000	60.0'	100.0'	60.0'	87	8408	6868	65.1'	100.0'	50.2'
42	6588	5688	65.0'	100.0'	50.0'	88	6000	6000	60.0'	100.0'	60.0'
43	6366	5638	65.0'	100.0'	40.0'	89	6000	6000	60.0'	100.0'	60.0'
44	8000	6000	60.0'	100.0'	60.0'	90	6000	6000	60.0'	100.0'	60.0'
45	8000	6000	60.0'	100.0'	60.0'	LOT 'A'	3336	2338	26.0'	118.8'	30.0'
46	8000	6000	60.0'	100.0'	60.0'	LOT 'B'	72117	72117	781.4'	100.0'	--

TOTAL LOT AREA (TM 38847): 721,281 SQ. FT. (16.36 AC. NET)
TOTAL AREA (TM 38847): 854,309 SQ. FT. (22.60 AC. NET)
"REMAINDER LOT" AREA: 1,260,724 SQ. FT. (28.94 AC. NET)
GRAND TOTAL AREA: 2,245,033 SQ. FT. (51.54 AC. NET)



1" = 60'
0 60 120 180
CITY CASE No. 16-05216

TENTATIVE MAP
TRACT 36647
CONCEPTUAL GRADING PLAN AND LOT MATRIX

SCALE: 1" = 60'	DATE: 8/23/2011	DRAWN BY: J. G. GIBSON	SHEET: 2
C&G: []	S/W: []	SWR: []	SD: []
CURB AND GUTTER	SIDEWALK	SEWER	WATER
STORM DRAIN			

DOWNEY CONSULTANTS
2708 HUNTER STREET
PERRIS, CA 92404
(951) 251-1270
FAX (951) 798-1258
DWG. NO.

LEGEND
C&G
S/W
SWR
SD

PARCEL D LOTLINE ADJ. NO. 18-06074

SEE SHEET 3

PERRY STREET

TENTATIVE TRACT MAP No. 36647 CONCEPTUAL GRADING

PERRIS VALLEY STORM DRAIN CHANNEL
(PER RCF&WCD DWG NO. 4-1066)

APN 302-130-003
GENERAL PLAN LAND USE: PUBLIC
ZONING: OPEN SPACE

APN 302-140-010
GENERAL PLAN LAND USE: SPECIFIC PLAN
ZONING: OPEN SPACE

APN 302-140-007

LOS ANGELES VINEYARD
ASSOCIATION TRACT 1 M.B. 10/29

"REMAINDER LOT"
CONTIGUOUS OWNERSHIP

APN 302-140-008

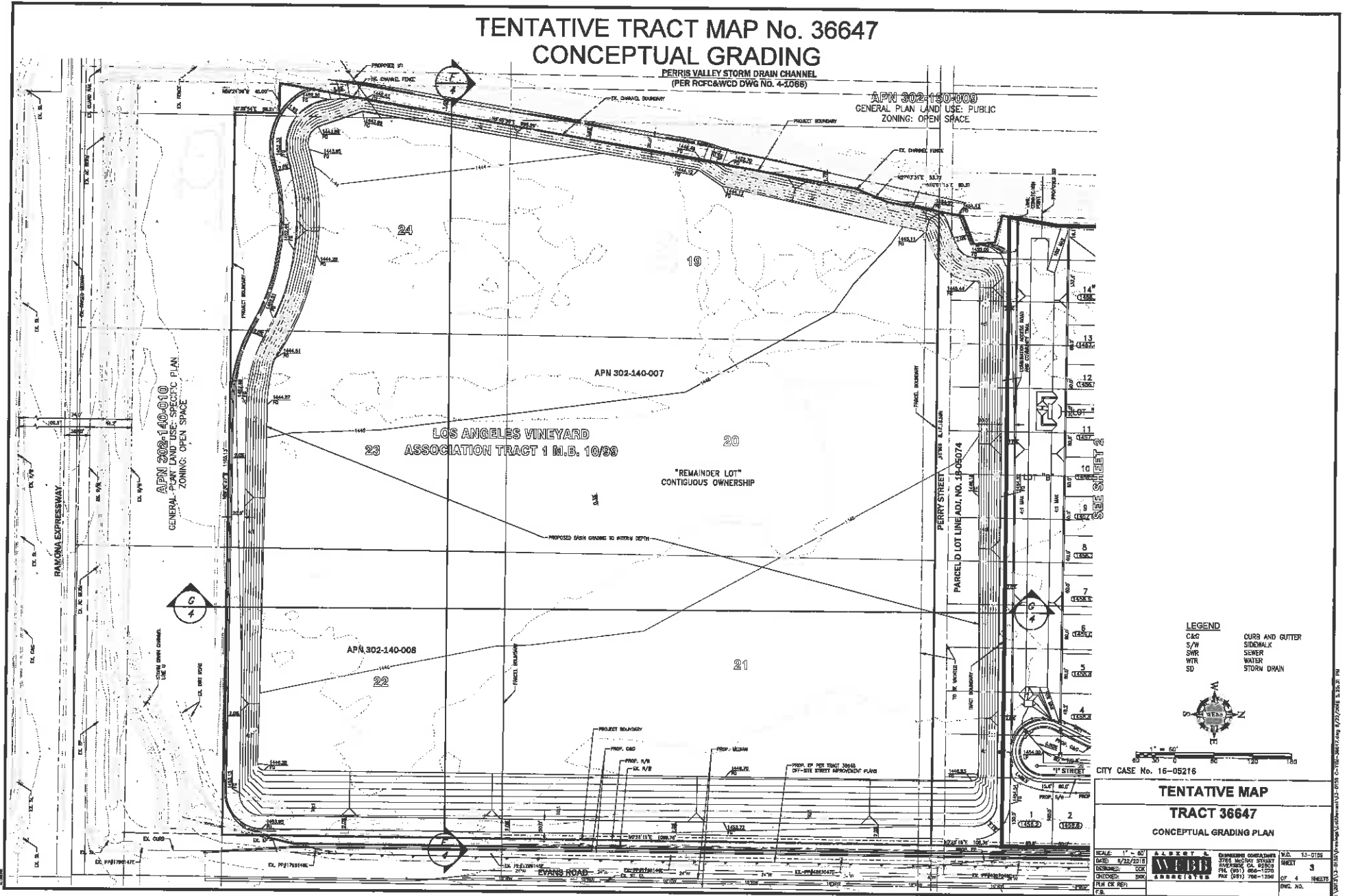
- LEGEND**
- C&G CURB AND GUTTER
 - S/W SIDEWALK
 - SWR SEWER
 - WTR WATER
 - SD STORM DRAIN



CITY CASE No. 16-05216

TENTATIVE MAP
TRACT 36647
CONCEPTUAL GRADING PLAN

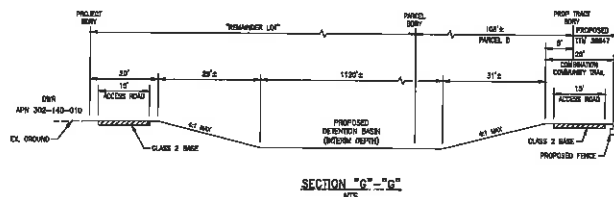
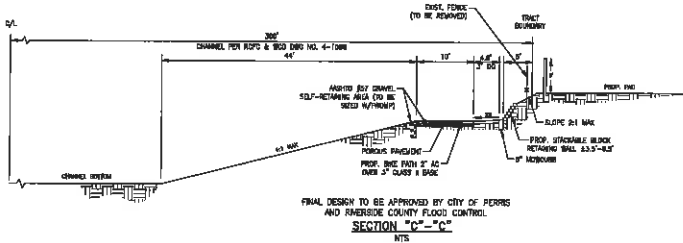
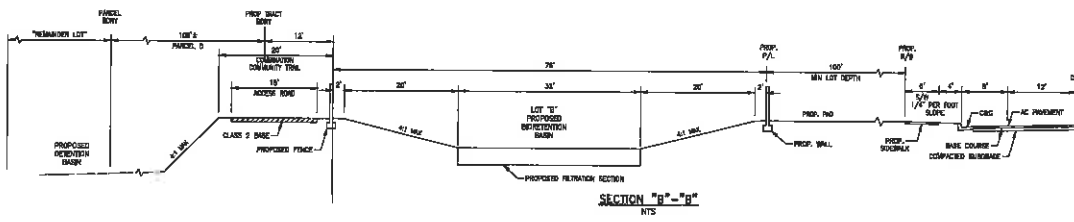
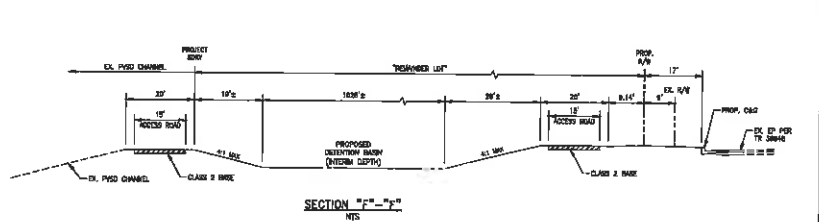
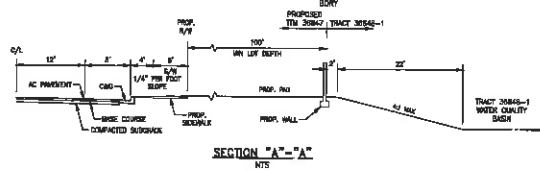
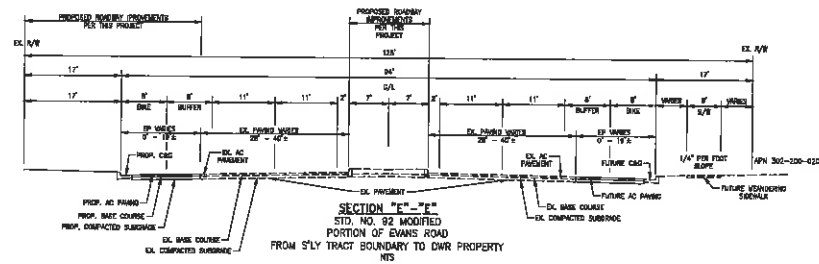
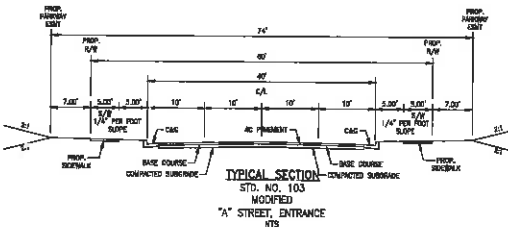
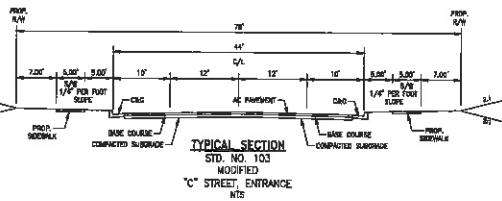
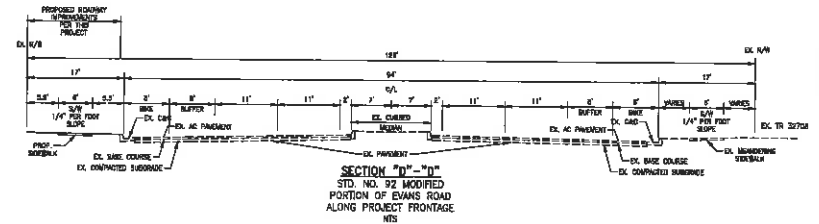
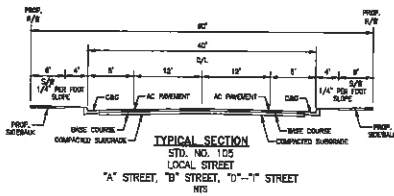
SCALE: 1" = 60'	DATE: 1/22/2016	SHEET: 3	M.A. 13-019
DESIGNED: EFC	DRAWN: SKM	OF 4 SHEETS	
PLN CK REP:		ENG. NO.	



TENTATIVE TRACT MAP No. 36647 STREET AND GRADING SECTIONS

NOTE
PROPOSED ROADWAYS SHALL BE DESIGNED, CONSTRUCTED, AND MAINTAINED TO SUPPORT A TOTAL WEIGHT OF 68,000 LBS.

NOTE
ALL STREET CUL-DE-SACS TO BE CONSTRUCTED PER COUNTY OF RIVERSIDE STANDARD 800. ALL OFFSET CUL-DE-SACS TO BE CONSTRUCTED PER COUNTY OF RIVERSIDE STANDARD 800(A). AND ALL STREET KNUCKLES TO BE CONSTRUCTED PER COUNTY OF RIVERSIDE STANDARD 801.



CITY CASE No. 16-05216

TENTATIVE MAP
TRACT 36647
STREET AND GRADING SECTIONS

SCALE: NTS	DATE: 8/25/2016	DESIGNER: CCA	CHECKED: SDC	FILED: CCB	DATE: 8/25/2016	SHEET: 4

M.D. 13-0169
SHEET 4 OF 4 SHEETS
D.M.C. INC.

NOTICE OF PUBLIC HEARING RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Thursday from 8:00 a.m. to 4:30 p.m., except Monday, October 8 (Columbus Day), and by prescheduled appointment on Fridays, from 9:00 a.m. to 5:00 p.m.

ATTENTION: ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. The City of Perris will hold hearings on this item and should be contacted on non-ALUC issues.

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside California

DATE OF HEARING: October 11, 2018

TIME OF HEARING: 9:30 A.M.

CASE DESCRIPTION:

ZAP1326MA18 – Stratford Ranch Investors, LLC (Representative: Mission Pacific Land Company, Jason Keller) – City of Perris Case Nos. 16-05217 (General Plan Amendment), 16-05218 (Zone Change), 16-05216 (Tentative Tract Map No. 36647). The applicant proposes to subdivide 24.06 gross acres located westerly of Evans Road, northerly of Ramona Expressway, and easterly of Oleander Avenue into 90 single family residential lots, amend the site's General Plan land use designation from Specific Plan (SP) to Single Family Residential R-6,000, and change its zoning from Single Family Residential R-10,000 to Single Family Residential R-6,000. (Airport Compatibility Zone D of the March Air Reserve Base/Inland Port Airport Influence Area).

FURTHER INFORMATION: Contact ALUC Planner Paul Rull at (951) 955-6893 or John Guerin at (951) 955-0982. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Mr. Nathan Perez of the City of Perris at (951) 943-5003.



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP1326MA18 DATE SUBMITTED: 8-14-18

APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

Applicant	Stratford Ranch Investors, LLC	Phone Number (949)333-6752
Mailing Address	4100 Newport Place	Email
	Suite 480	
	Newport Beach, CA 92660	
Representative	Jason Keller	Phone Number (949)333-6752
Mailing Address	Same as above	Email jkeller@missionpacific.com
Property Owner	Stratford Ranch Investors, LLC	Phone Number (949)333-6752
Mailing Address	Same as above	Email jabel@missionpacific.com

LOCAL JURISDICTION AGENCY

Local Agency Name	City of Perris	Phone Number (951) 943-5003
Staff Contact	Nathan Perez	Email nperez@cityofperris.org
Mailing Address	135 North D Street	Case Type
	Perris, CA 92570	<input checked="" type="checkbox"/> General Plan / Specific Plan Amendment
		<input checked="" type="checkbox"/> Zoning Ordinance Amendment
		<input checked="" type="checkbox"/> Subdivision Parcel Map / Tentative Tract
Local Agency Project No	TTM 36647 (16-05216), ZC (16-05216), GPA (16-05217)	<input type="checkbox"/> Use Permit
		<input type="checkbox"/> Site Plan Review/Plot Plan
		<input type="checkbox"/> Other

PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address	Evans Road, North of Ramona Expressway, South of Marbella Gate		
Assessor's Parcel No.	302-150-009 through 019	Gross Parcel Size	24.06 acres
Subdivision Name	Stratford Ranch Tract 36647	Nearest Airport and distance from Airport	March Field -
Lot Number	n/a		

PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe)	Undeveloped, Dry Farmed

March
D

Proposed Land Use (describe)	Subdivide 24.06 acres into 90 single family residential lots. Minimum lot size to be 6,000 sf. Project proposes to grade an approximately 28-acre offsite drainage basin on the adjacent property.		
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)	90 SFR Lots	
For Other Land Uses (See Appendix C)	Hours of Operation	n/a	
	Number of People on Site	780	Maximum Number
	Method of Calculation	300 sf per Occupant for Dwellings per Appendix C	
Height Data	Site Elevation (above mean sea level)	Highest Pad Elevation = 1460.6'	ft.
	Height of buildings or structures (from the ground)	30'	ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?		<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
	If yes, describe _____ _____ _____		

- A. **NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. **REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. **SUBMISSION PACKAGE:**
- ✓ 1. Completed ALUC Application Form
 - ✓ 1. ALUC fee payment
 - ✓ 1. Plans Package (24x36 folded) (site plans, floor plans, building elevations, landscaping plans, grading plans, subdivision maps)
 - ✓ 1. Plans Package (8.5x11) (site plans, floor plans, building elevations, landscaping plans, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
 - 1. CD with digital files of the plans (pdf)
 - ✓ 1. Vicinity Map (8.5x11)
 - ✓ 1. Detailed project description
 - ✓ 1. Local jurisdiction project transmittal
 - ✓ 3. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
 - ✓ 3. Gummed address labels of all surrounding property owners within a 300 foot radius of the project site (**only required if the project is scheduled for a public hearing Commission meeting**). If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10) with ALUC return address. *

* Projects involving heliports/helicopter landing sites will require additional noticing procedures.

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.5

HEARING DATE: October 11, 2018

CASE NUMBER: ZAP1327MA18 – Mobilitie, LLC (Representative: Robert Schultz)

APPROVING JURISDICTION: March Joint Powers Authority

JURISDICTION CASE NO: CUP18-01 (Conditional Use Permit)

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends that the Commission find the proposed Conditional Use Permit **CONDITIONALLY CONSISTENT**, subject to the conditions included herein, and such additional conditions as may be required by the Federal Aviation Administration Obstruction Evaluation Service.

PROJECT DESCRIPTION: The applicant proposes to remove an existing 26 foot tall street light pole and replace it with a new 33.3 foot tall wireless facility street light pole (and associated equipment) within the street right-of-way of Innovation Drive.

PROJECT LOCATION: The site is located on the westerly side of Innovation Drive (within the street right-of-way), southerly of Cactus Avenue, within the land use jurisdiction of the March Joint Powers Authority, approximately 6,000 feet northwesterly of the northerly end of Runway 14-32 at March Air Reserve Base.

LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

- a. Airport Influence Area: March Air Reserve Base
- b. Land Use Policy: Zone B2
- c. Noise Levels: 65-70 CNEL from aircraft

BACKGROUND:

Non-Residential Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zone B2. Zone B2 criteria restrict non-residential intensity to 100 people per acre, and 250 people per single acre. The proposed project is an unmanned wireless facility with no onsite occupancy.

March Air Reserve Base/United States Air Force Input: Given that the project site includes land located within Airport Compatibility Zone B2, the March Air Reserve Base staff was notified of the project and sent a package of plans for their review. As of the time this staff report was prepared (September 18), we were still awaiting comments from the Air Force regarding this project.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zone B2 (children's schools, day care centers, hospitals, nursing homes, buildings with 3 aboveground floors, highly noise-sensitive outdoor nonresidential uses and hazards to flight). A wireless facility light pole is compatible with Zone B2. (The project is located outside the Accident Potential Zone; therefore, the 2018 Air Installation Compatibility Use Zone [AICUZ] criteria regarding permitted land uses are not applicable.)

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being in an area within the 65-70 CNEL range from aircraft noise. The project will not generate any occupancy that could be affected by aircraft-generated noise.

Part 77: The elevation of Runway 14-32 at its northerly terminus is 1,535 feet above mean sea level (1,535 feet AMSL). At a distance of approximately 6,000 feet from the runway to the site, Federal Aviation Administration (FAA) review would be required for any structures with top point exceeding 1,595 feet AMSL. The site elevation is approximately 1,568 feet AMSL. Given the proposed structure height of 33.3 feet, the top point elevation would be 1,601.3 feet AMSL. Therefore, review of the structure by the FAA Obstruction Evaluation Service is required. The applicant has submitted Form 7460-1, and FAA OES has assigned Aeronautical Study No. 2018-AWP-14839-OE to this project. Its status is currently a "work in progress".

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

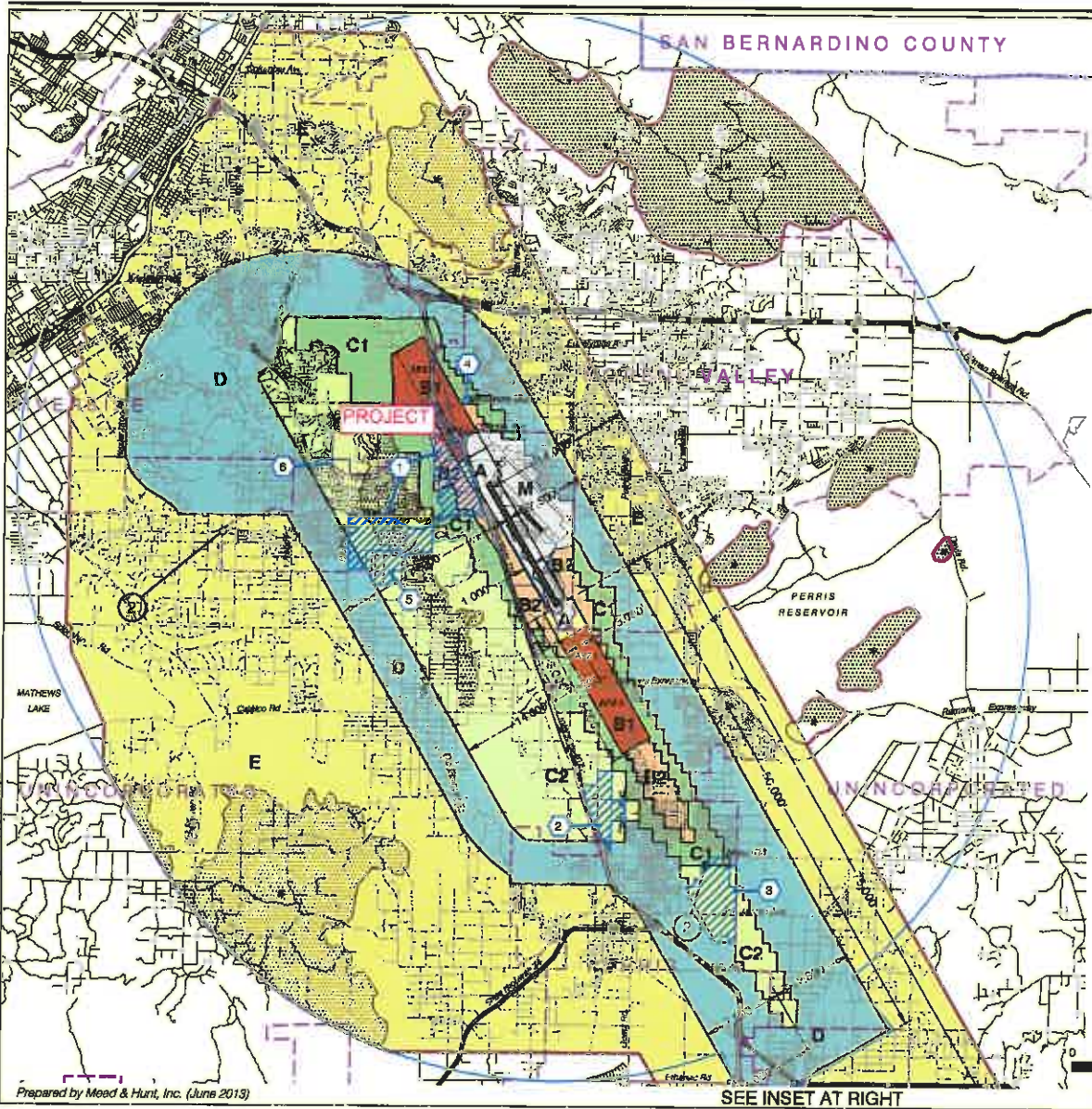
CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.

- (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



LEGEND

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)
- March JPA: March Business Center/Meridian
- Perris: Harvest Landing
- Perris: Park West
- Moreno Valley: Affordable Housing
- March JPA: Ben Clark Training Center
- Riverside: Ridge Crest Subdivision

- ① Point at which aircraft on Runway 32 (LS approach) descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 5,000 feet above runway end.

INSET



**Riverside County
Airport Land Use Commission**

**March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan**

(Adopted November 13, 2014)

Map MA-1

Compatibility Map
March Air Reserve Base / Inland Port Airport

Note:
All dimensions are measured from
runway ends and centerlines.

N

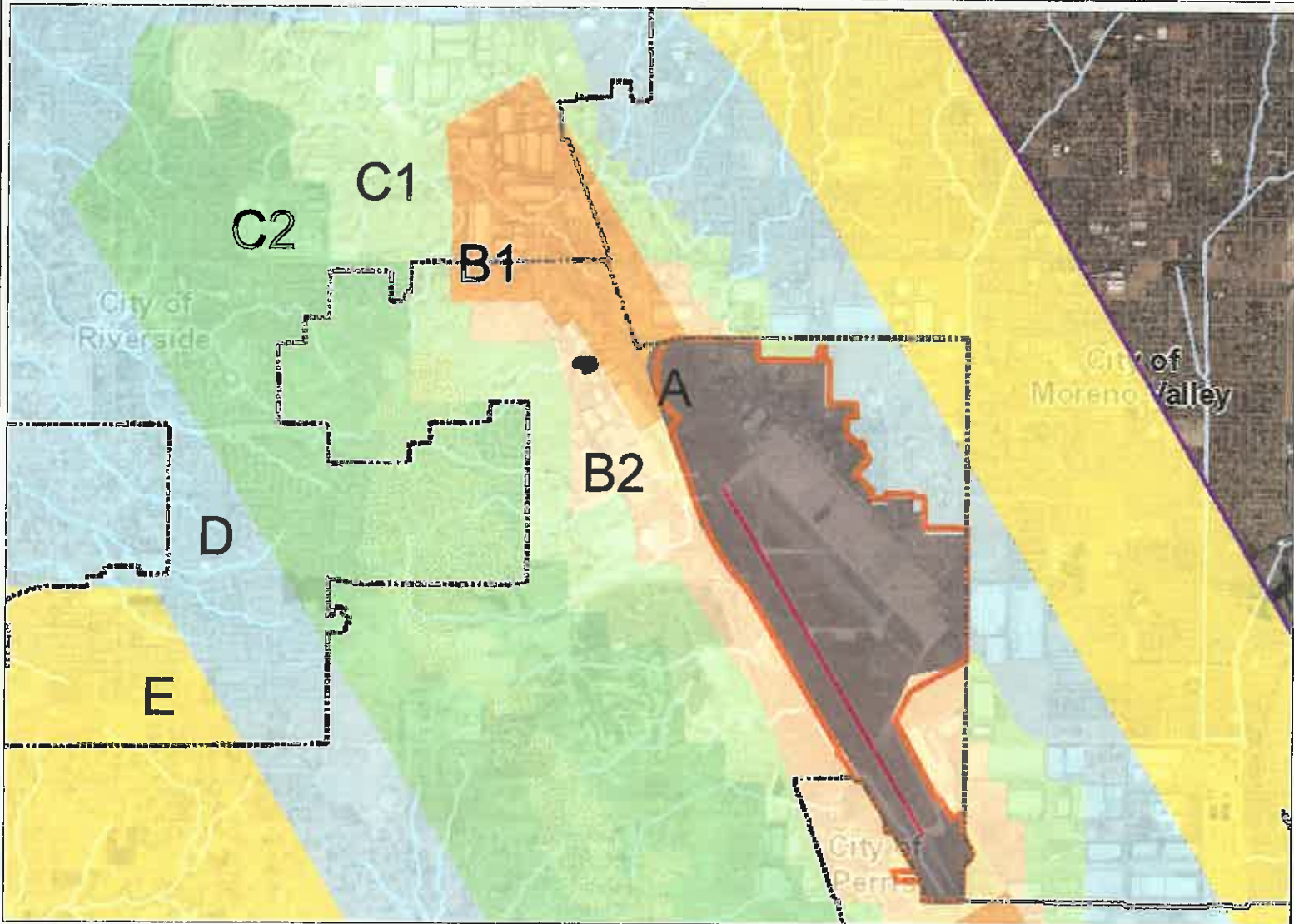
2 4 MILES

Base map source: County of Riverside 2013

Prepared by Mead & Hunt, Inc. (June 2013)



SEE INSET AT RIGHT

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- ▨ OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
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- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6

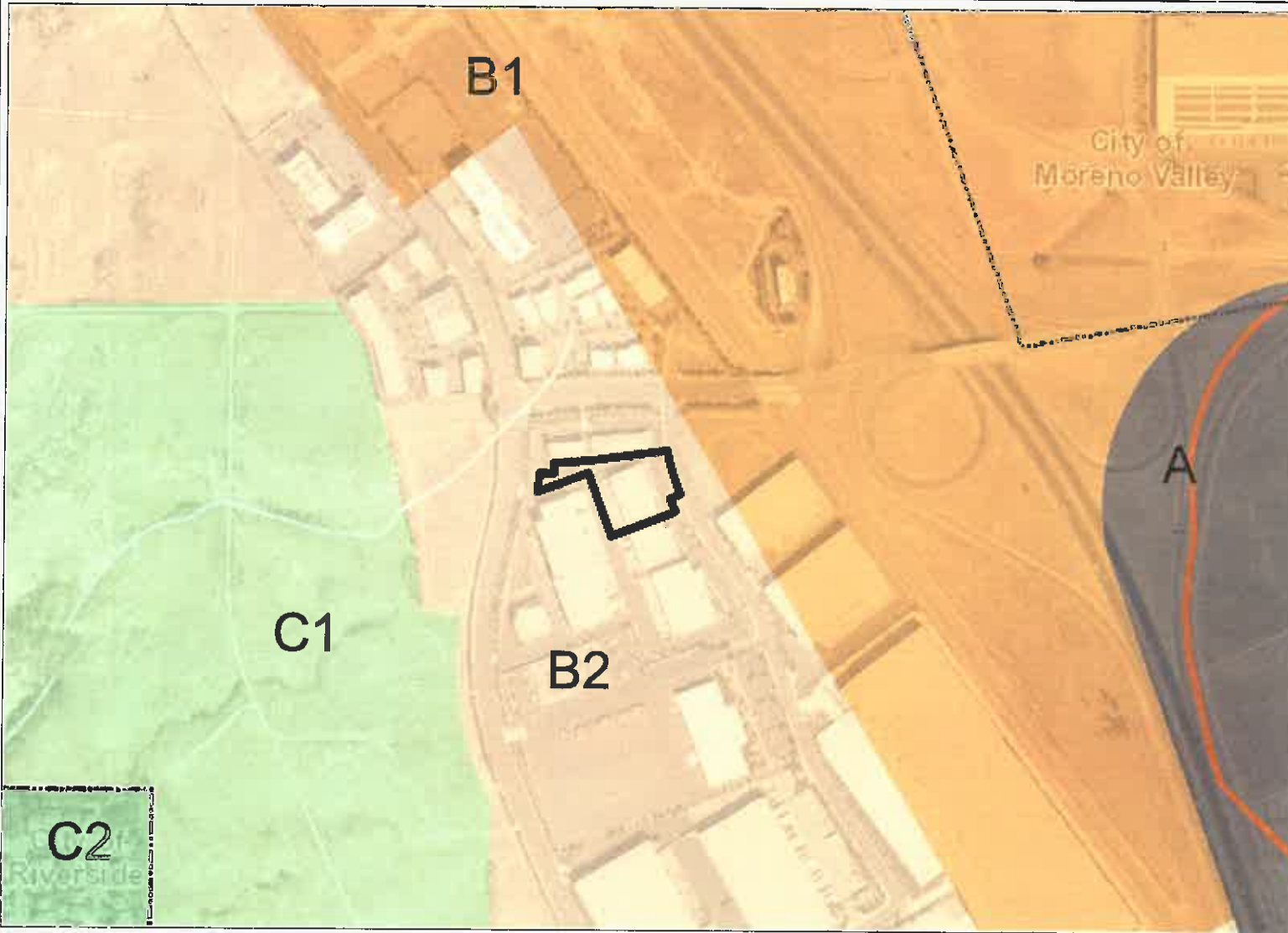



0 6 12,314 Feet

IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

Notes

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC8



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0 770 1,539 Feet

REPORT PRINTED ON... 8/27/2018 8:49:12 AM

© Riverside County GIS

Notes

Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map

Notes



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.






REPORT PRINTED ON... 8/27/2018 8:51:49 AM

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Map My County Map



Legend

-  Blue Line Streams
-  City Areas
-  World Street Map



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Notes

Map My County Map



- Legend**
- Blueline Streams
 - ▣ City Areas
 - World Street Map



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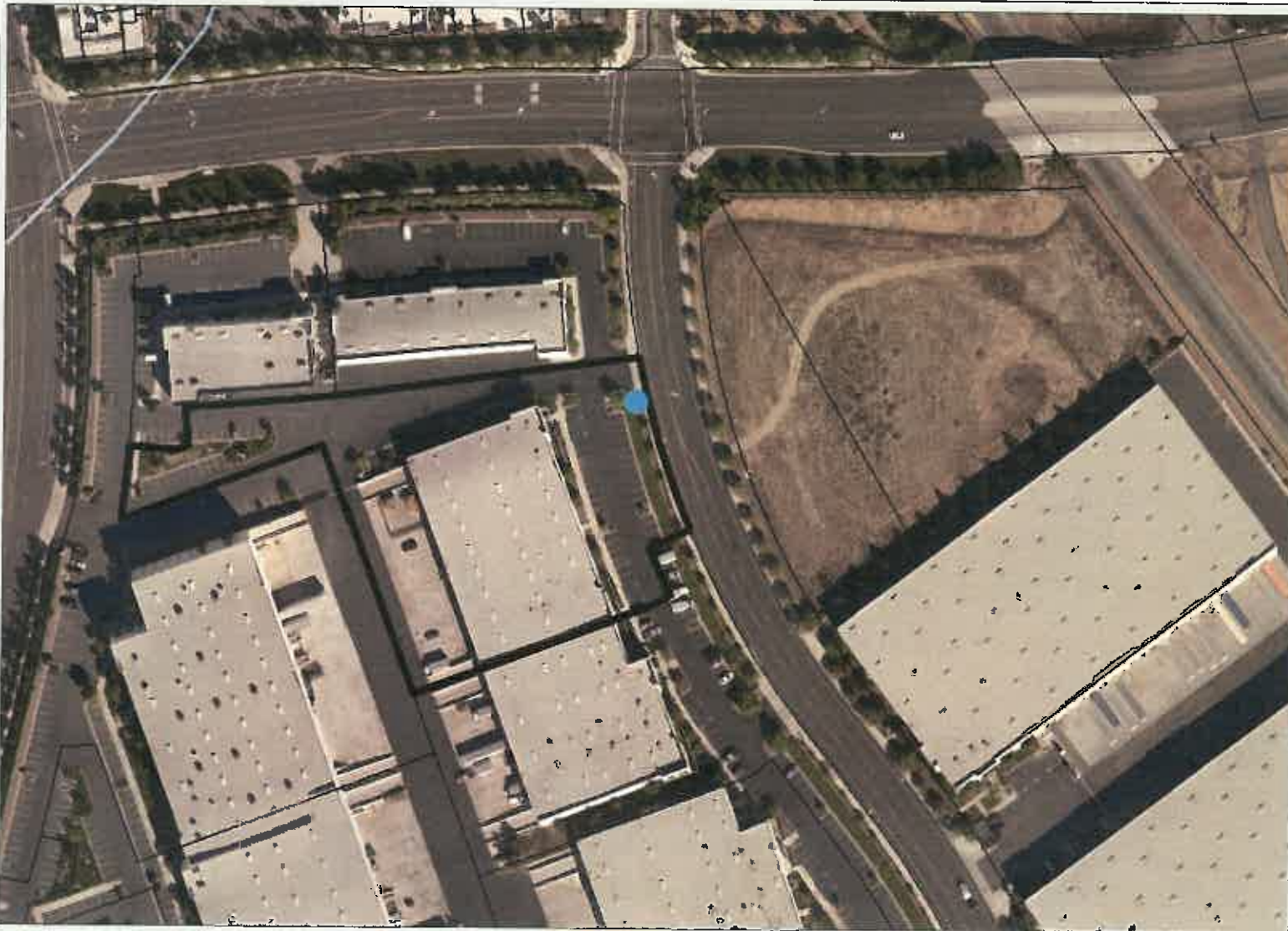


REPORT PRINTED ON... 8/27/2018 8:49:54 AM

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Notes

Map My County Map



Legend

- Parcels
- Blueline Streams
- City Areas
- World Street Map

Notes



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0 192 385 Feet

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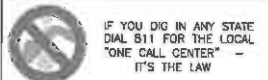
intelligent infrastructure

SITE ID-CANDIDATE LETTER/CASCADE ID-CANDIDATE LETTER:
9CAB012636D/RV90XSB08D

COORDINATES OF POLE LOCATION:
33.90776/-117.285062

CROSS STREET:
CACTUS AVE. & INNOVATION DR.
 CITY, STATE ZIP:
RIVERSIDE, CA 92508

11"x17" PLOT WILL BE HALF SCALE UNLESS OTHERWISE NOTED



THE UTILITIES SHOWN HEREIN ARE FOR THE CONTRACTORS CONVENIENCE ONLY. THERE MAY BE OTHER UTILITIES NOT SHOWN ON THESE PLANS. THE ENGINEER/SURVEYOR ASSUMES NO RESPONSIBILITY FOR THE LOCATIONS SHOWN AND IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL THE UTILITIES WITHIN THE LIMITS OF THE WORK. ALL DAMAGE MADE TO THE (S) UTILITIES BY THE CONTRACTOR SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR.

GENERAL NOTES

THE FACILITY IS UNMANNED AND NOT FOR HUMAN HABITATION. A TECHNICIAN WILL VISIT THE SITE AS REQUIRED FOR ROUTINE MAINTENANCE. THE PROJECT WILL NOT RESULT IN ANY SIGNIFICANT DISTURBANCE OF EFFECT ON DRAINAGE; NO SANITARY SEWER SERVICE, POTABLE WATER OR TRASH DISPOSAL IS REQUIRED AND NO COMMERCIAL SIGNAGE IS (N).

SITE INFORMATION

SITE ID:	9CAB012636D
CASCADE ID:	RV90XSB08D
LATITUDE:	33.90776
LONGITUDE:	-117.285062
CROSS STREET:	CACTUS AVE. & INNOVATION DR.
CITY, STATE, ZIP:	RIVERSIDE, CA 92508
COUNTY:	RIVERSIDE COUNTY
JURISDICTION:	CITY OF RIVERSIDE
PROPERTY OWNER:	PUBLIC RIGHT-OF-WAY
APPLICANT:	MOBILITE, LLC 2955 RED HILL AVENUE, STE. 200 COSTA MESA, CA 92626 CONTACT: ROBERT LEWIS PHONE: 951.212.5825 EMAIL: rlewis@mobilitie.com

ENGINEER

XD INDUSTRIES 1572 N. BATAVIA ST. SUITE 1D ORANGE, CA 92676	JOSE FRIAS (714) 847-3093 PROJECT NUMBER: 9CAB012636D
--	--

DO NOT SCALE DRAWINGS

CONTRACTORS SHALL VERIFY ALL PLANS, (E) DIMENSIONS & FIELD CONDITIONS ON THE JOB SITE & SHALL IMMEDIATELY NOTIFY THE ARCHITECT/ENGINEER IN WRITING OF ANY DISCREPANCIES BEFORE PROCEEDING WITH THE WORK OR BE RESPONSIBLE FOR SAME.

LOCATION MAPS



PROJECT DESCRIPTION

END USER PROPOSES TO REPLACE EXISTING CONCRETE POLE AND INSTALL EQUIPMENT ON A NEW CONCRETE POLE WITHIN AN EXISTING RIGHT-OF-WAY.

THE SCOPE WILL CONSIST OF THE FOLLOWING:

- REMOVE EXISTING CONCRETE LIGHT POLE
- INSTALL A NEW CONCRETE LIGHT POLE WITH PROPOSED BACK-HAUL TRANSPORT EQUIPMENT

CODES

2015 INTERNATIONAL BUILDING CODE GENERAL ORDER 95
 2014 NATIONAL ELECTRICAL CODE
 TIA/EIA-222-G-2 OR LATEST EDITION
 LOCAL BUILDING/PLANNING CODE

DRAWING INDEX

SHEET NO:	SHEET TITLE
T-1	TITLE SHEET
GN-1	GENERAL NOTES
GN-2	GENERAL NOTES
GN-3	GENERAL NOTES
SP-1	EXHIBIT PHOTO & SITE PLAN
SP-2	OVERALL SITE PLAN
EV-1	POLE ELEVATIONS
EV-2	POLE ELEVATIONS
PL-1	PLUMBING & RISER DIAGRAM
EQ-1	EQUIPMENT DETAILS
EQ-2	EQUIPMENT DETAILS
EQ-3	POLE SPECIFICATIONS
EX-1	EXCAVATION DETAILS
E-1	ELECTRICAL
G-1	GROUNDING DETAILS
TC-1	VEHICULAR TRAFFIC CONTROL PLAN



PROJECT NO: RV90XSB08D
 DRAWN BY: MR
 CHECKED BY: JF

1 07/16/18 CLIENT COMMENTS
 0 07/25/18 CONSTRUCTION DRAWINGS

IT IS A VIOLATION OF THE LAW FOR ANY PERSON UNLESS THEY ARE ACTING UNDER THE SUPERVISION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT

RV90XSB08D
 RIVERSIDE, CA 92608
 (N) CONCRETE LIGHT POLE

SHEET TITLE
TITLE SHEET

SHEET NUMBER
T-1



(E) 26'-0" CONCRETE LIGHT POLE (ID SL520767) TO BE REMOVED AND REPLACED

EXHIBIT PHOTO

SCALE: NOT TO SCALE

1



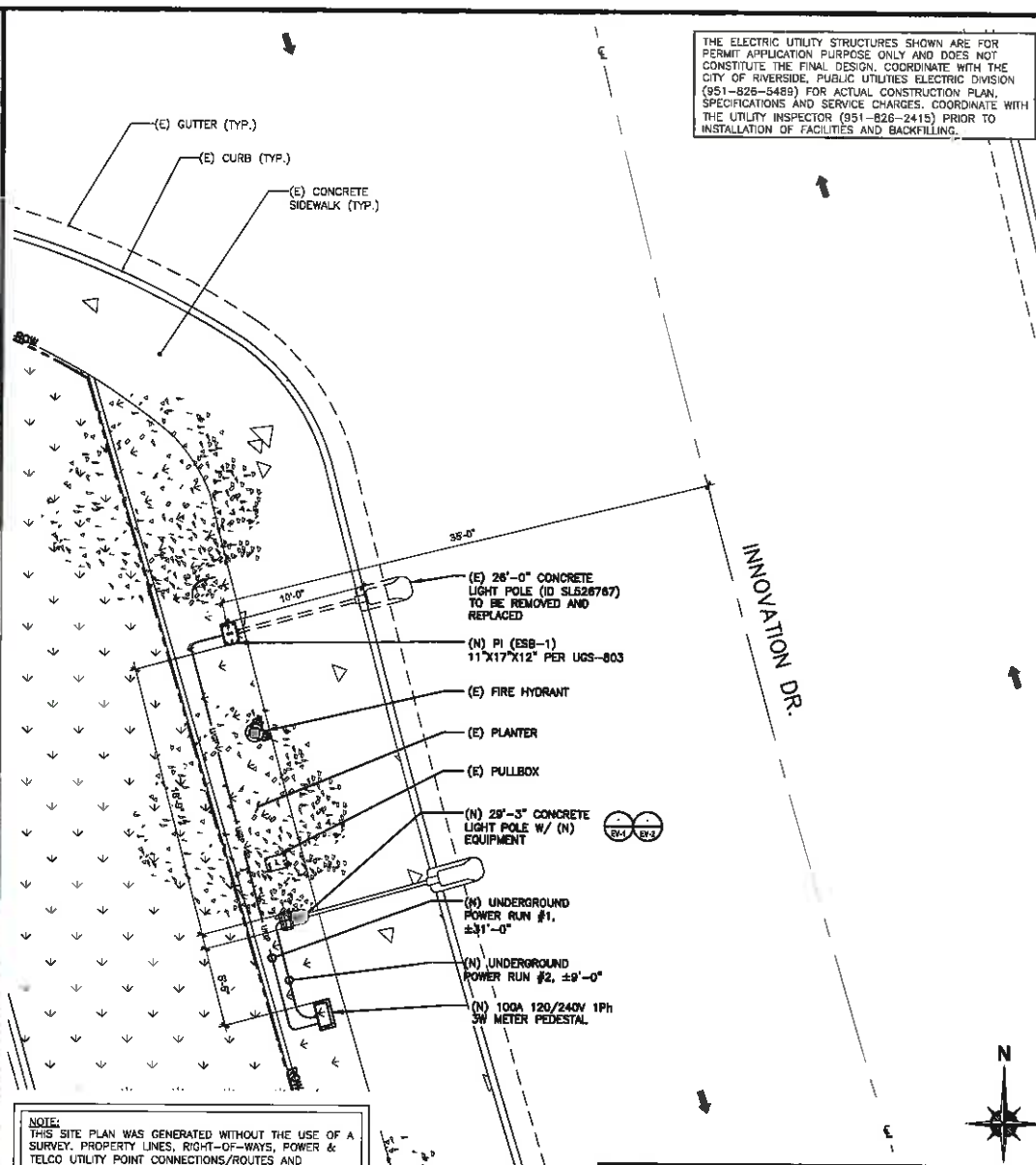
SITE LOCATION

WEST MARCH

AERIAL SITE LOCATION

SCALE: NOT TO SCALE

2




THE ELECTRIC UTILITY STRUCTURES SHOWN ARE FOR PERMIT APPLICATION PURPOSE ONLY AND DOES NOT CONSTITUTE THE FINAL DESIGN. COORDINATE WITH THE CITY OF RIVERSIDE, PUBLIC UTILITIES ELECTRIC DIVISION (951-826-5489) FOR ACTUAL CONSTRUCTION PLAN, SPECIFICATIONS AND SERVICE CHARGES. COORDINATE WITH THE UTILITY INSPECTOR (951-826-2415) PRIOR TO INSTALLATION OF FACILITIES AND BACKFILLING.


NOTE:
THIS SITE PLAN WAS GENERATED WITHOUT THE USE OF A SURVEY. PROPERTY LINES, RIGHT-OF-WAYS, POWER & TELCO UTILITY POINT CONNECTIONS/ROUTES AND EASEMENTS SHOWN ON THESE PLANS ARE ESTIMATED. ALL ITEMS AND DIMENSIONS SHOULD BE VERIFIED IN THE FIELD.

ENLARGED SITE PLAN

SCALE: 1/8" = 1'-0" (1/4" = 1'-0" ON 22"x34" SHEET)

3





1232 N. SUTAWA ST., STE. 10
ORANGE, CA 92667

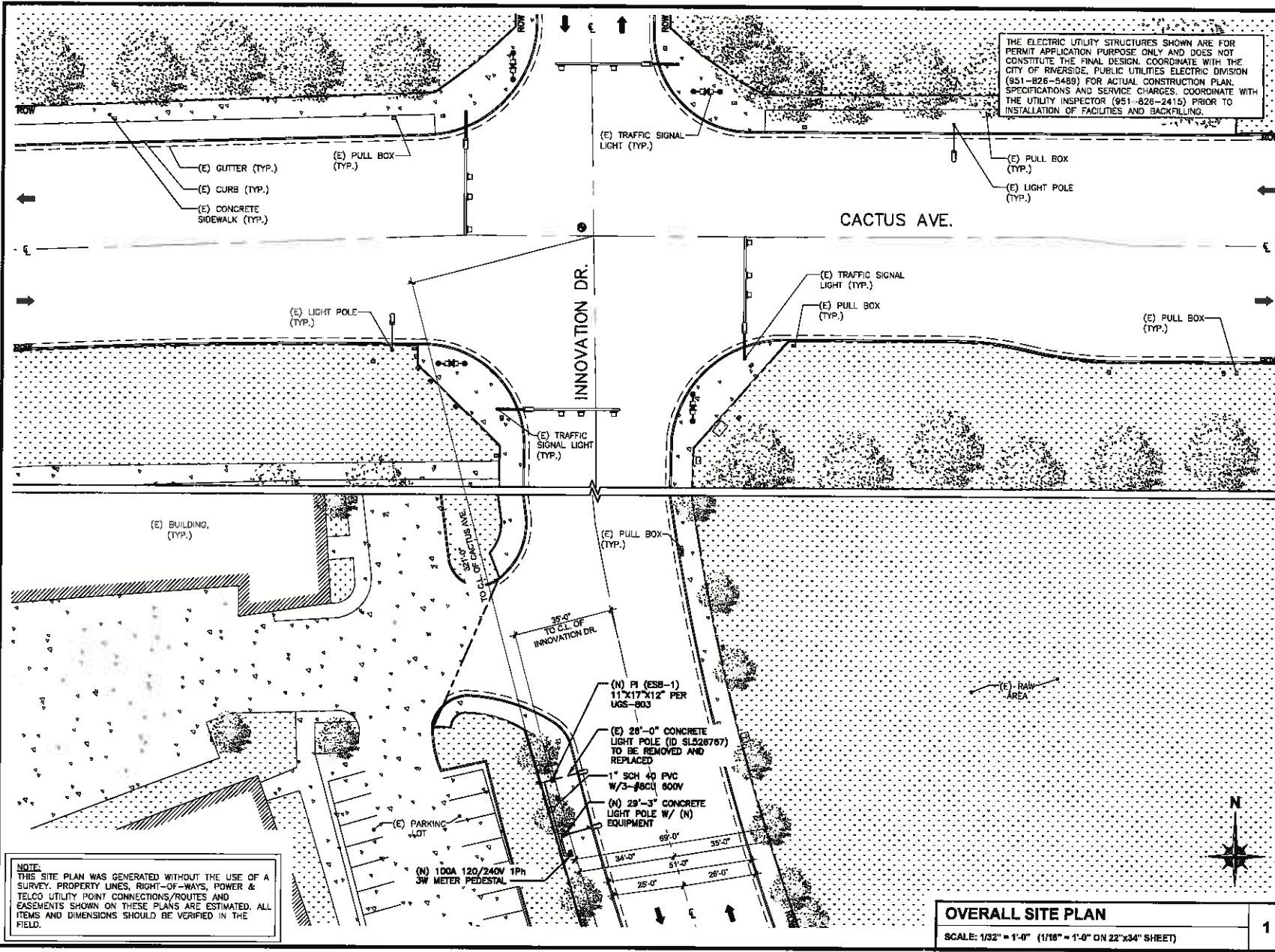
PROJECT NO:	RV90XSB00D
DRAWN BY:	MIR
CHECKED BY:	JF
1 07/18/16	CLIENT COMMENTS
2 07/26/16	CONSTRUCTION DRAWINGS

IT IS A VIOLATION OF THE LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT.

RV90XSB00D
RIVERSIDE, CA 92508
(N) CONCRETE LIGHT POLE

SHEET TITLE
EXHIBIT PHOTO & SITE PLAN

SHEET NUMBER
SP-1



NOTE:
 THIS SITE PLAN WAS GENERATED WITHOUT THE USE OF A SURVEY. PROPERTY LINES, RIGHT-OF-WAYS, POWER & TELCO UTILITY POINT CONNECTIONS/ROUTES AND EASEMENTS SHOWN ON THESE PLANS ARE ESTIMATED. ALL ITEMS AND DIMENSIONS SHOULD BE VERIFIED IN THE FIELD.

OVERALL SITE PLAN
 SCALE: 1/32" = 1'-0" (1/16" = 1'-0" ON 22"x34" SHEET)



PROJECT NO: RV90XSB08D
 DRAWN BY: MR
 CHECKED BY: JF

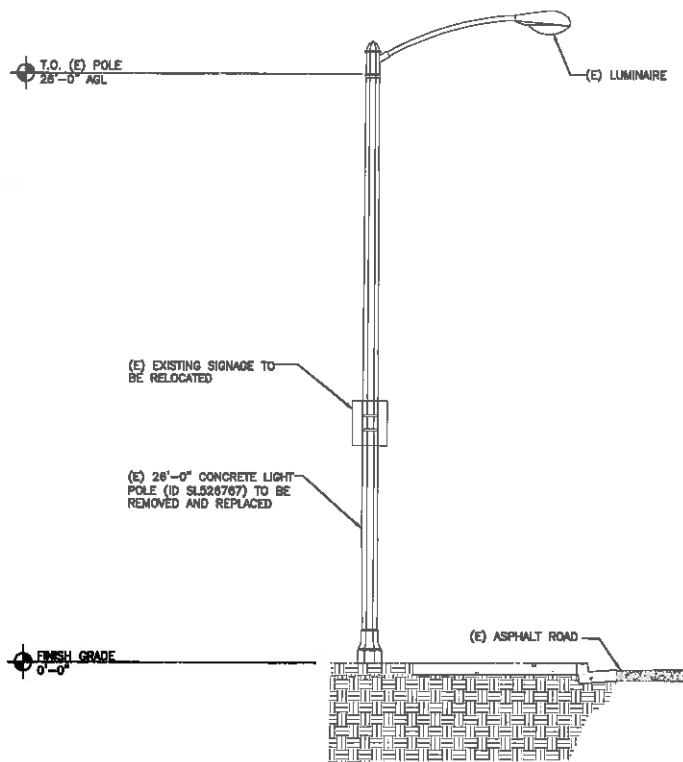
1	07/15/16	CLIENT COMMENTS
2	07/25/16	CONSTRUCTION DRAWINGS

IF IS A VIOLATION OF THE LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT

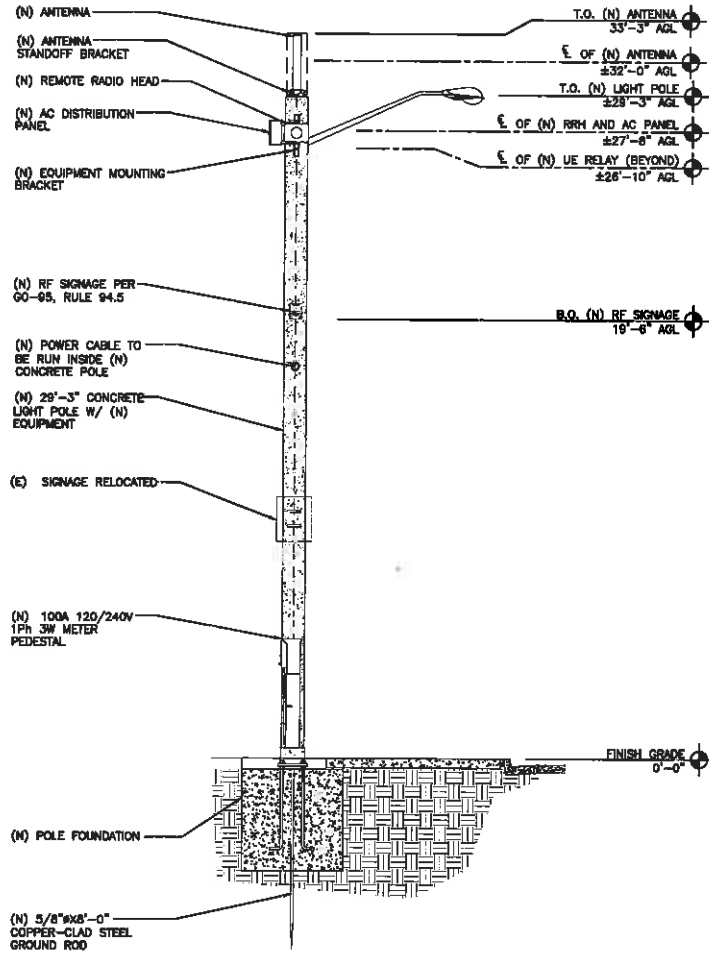
RV90XSB08D
 RIVERSIDE, CA 92508
 (N) CONCRETE LIGHT POLE
 SHEET TITLE
OVERALL SITE PLAN
 SHEET NUMBER
SP-2

NOTES:

1. ALL HARDWARE SHALL BE STAINLESS STEEL.
2. ALL CABLES SHALL BE SECURED TO POLE EVERY 36" OR LESS.
3. LIGHTNING RODS SHALL BE INCLUDED AS REQUIRED.
4. STRUCTURAL BACKFILL TO BE COMPACTED IN 6" MAXIMUM LAYERS TO 95% OF CONTENT IN ACCORDANCE WITH ASTM D698. ADDITIONALLY, STRUCTURAL BACKFILL MUST HAVE A MINIMUM COMPACTED UNIT WEIGHT OF 100 POUNDS PER CUBIC FOOT (16kN/m³)



EXISTING SIDE VIEW



NEW SIDE VIEW

POLE ELEVATIONS

SCALE: 3/16" = 1'-0" (3/8" = 1'-0" ON 22"x34" SHEET)

1



PROJECT NO: RV80XSB06D
 DRAWN BY: MR
 CHECKED BY: JF

1	07/28/18 CLIENT COMMENTS
0	07/25/18 CONSTRUCTION DRAWINGS

IT IS A VIOLATION OF THE LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT

RV80XSB06D
 RIVERSIDE, CA 92508
 (N) CONCRETE LIGHT POLE

SHEET TITLE
 POLE ELEVATIONS

SHEET NUMBER
 EV-1

NOTES:

1. ALL HARDWARE SHALL BE STAINLESS STEEL.
2. ALL CABLES SHALL BE SECURED TO POLE EVERY 36" OR LESS.
3. LIGHTNING RODS SHALL BE INCLUDED AS REQUIRED.
4. STRUCTURAL BACKFILL TO BE COMPACTED IN 6" MAXIMUM LAYERS TO 95% OF CONTENT IN ACCORDANCE WITH ASTM D888. ADDITIONALLY, STRUCTURAL BACKFILL MUST HAVE A MINIMUM COMPACTED UNIT WEIGHT OF 100 POUNDS PER CUBIC FOOT (16kN/m³)



PROJECT NO: RV90XB08D
 DRAWN BY: MR
 CHECKED BY: JF

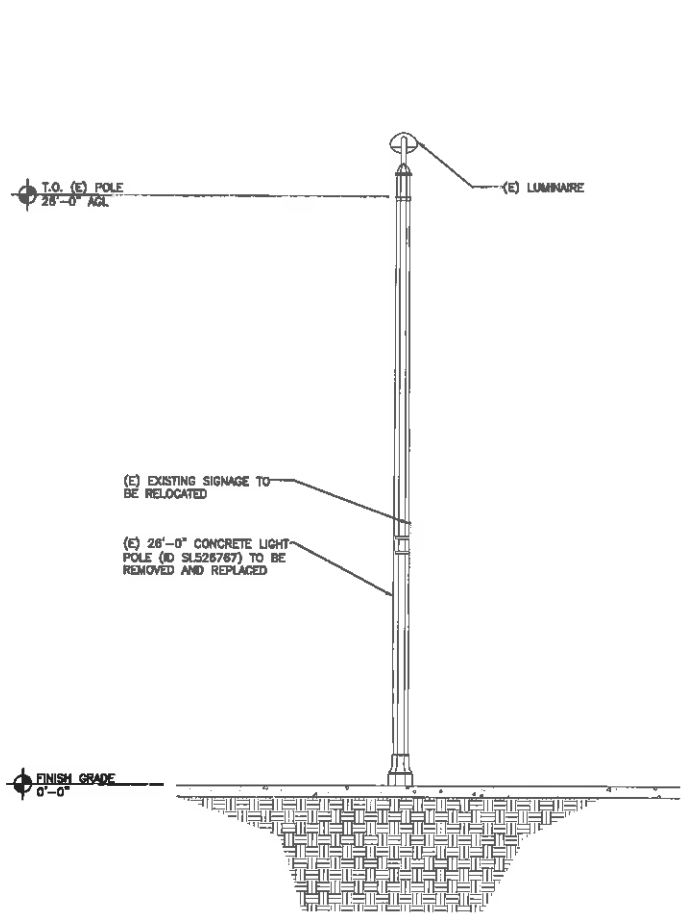
1 07/16/18 CLIENT COMMENTS
 0 07/25/18 CONSTRUCTION DRAWINGS

IT IS A VIOLATION OF THE LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT

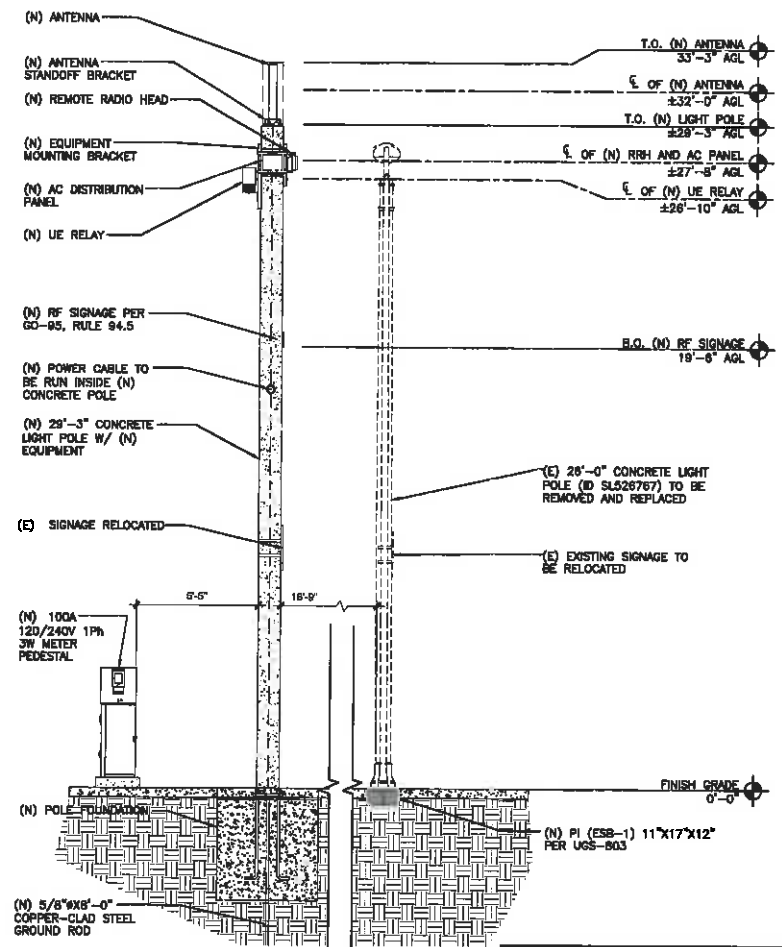
RV90XB08D
 RIVERSIDE, CA 92508
 (N) CONCRETE LIGHT POLE

SHEET TITLE
 POLE ELEVATIONS

SHEET NUMBER
 EV-2

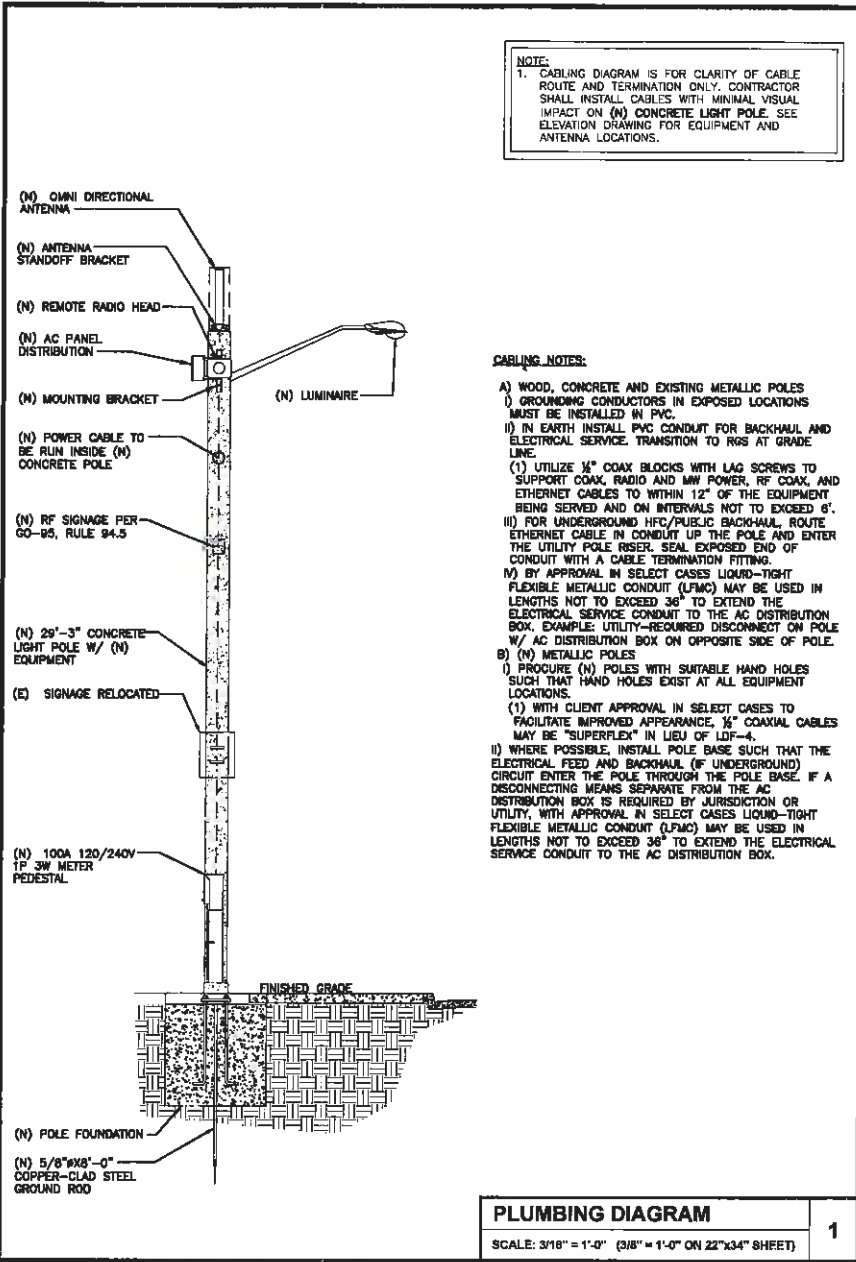


EXISTING FRONT VIEW



NEW FRONT VIEW

POLE ELEVATIONS
 SCALE: 3/16" = 1'-0" (3/8" = 1'-0" ON 22"x34" SHEET)



BILL OF MATERIALS

QTY.	DESCRIPTION	MANUFACTURER	MODEL NUMBER	AZMUTH	CABLE	DIMENSIONS (HWWD)	WEIGHT
1	ANTENNA	ALPHA WIRELESS	AW3477-S-G			30.7" X 4.7" DIAMETER	7 LBS
1	UE RELAY	AIRSPAN	RR460-SPB-ST-1-P-0			13" X 7" DIAMETER	8.8 LBS
1	RADIO	NOKIA	473548A			8.7" X 12.9" X 6.3"	24.7 LBS
1	AC DISTRIBUTION PANEL	RAYCAP	RSTAC-3112-P-120			9.25" X 9.5" X 3.81"	8 LBS
1	MMS SHROUD ENCLOSER	ELTEK				35" X 15.5" X 9"	TBD
1	METER PEDESTAL	MILBANK	CP301110A22SL			1'x 48" X 17.25" X 18.25"	TBD

BILL OF MATERIALS 2
SCALE: NOT TO SCALE

mobilitte
Intelligent Infrastructure

XD Industries
1873 N. BAYVIEW ST., STE. 10
ORANGE, CA 92667

PROJECT NO: RV90X8B00D
DRAWN BY: MR
CHECKED BY: JF

1 07/8/18 CLIENT COMMENTS
0 07/26/18 CONSTRUCTION DRAWINGS

IT IS A VIOLATION OF THE LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT

RV90X8B00D
RIVERSIDE, CA 92608
(N) CONCRETE LIGHT POLE

SHEET TITLE
PLUMBING & RISER DIAGRAM

SHEET NUMBER
PL-1

NOT USED 3
SCALE: NOT TO SCALE

GENERAL CONSTRUCTION NOTES:

1. ALL WORK SHALL CONFORM TO THE REQUIREMENTS OF THE LOCAL BUILDING CODE, THE LATEST ADOPTED EDITION AND ALL OTHER APPLICABLE CODES AND ORDINANCES.
2. CONTRACTOR SHALL CONSTRUCT SITE IN ACCORDANCE WITH THESE DRAWINGS AND LATEST MOBILITE CONSTRUCTION STANDARDS. THE SPECIFICATION IS THE RULING DOCUMENT AND ANY DISCREPANCIES BETWEEN THE SPECIFICATION AND THE CONSTRUCTION DRAWINGS SHALL BE BROUGHT TO THE ATTENTION OF THE ARCHITECT/ENGINEER OR MOBILITE CM PRIOR TO THE COMMENCEMENT OF WORK.
3. CONTRACTOR SHALL VISIT THE JOB SITE AND SHALL FAMILIARIZE THEMSELVES WITH ALL CONDITIONS AFFECTING THE NEW WORK AND SHALL MAKE PROVISIONS AS TO THE COST THEREOF. CONTRACTOR SHALL BE RESPONSIBLE FOR FAMILIARIZING THEMSELVES WITH ALL CONTRACT DOCUMENTS, FIELD CONDITIONS AND DIMENSIONS AND CONFIRMING THAT THE WORK MAY BE ACCOMPLISHED, AS SHOWN, PRIOR TO PROCEEDING WITH CONSTRUCTION. ANY DISCREPANCIES SHALL BE BROUGHT TO THE ATTENTION OF THE ARCHITECT/ENGINEER OR MOBILITE CM PRIOR TO THE COMMENCEMENT OF WORK. NO COMPENSATION WILL BE AWARDED BASED ON CLAIM OF LACK OF KNOWLEDGE OF FIELD CONDITIONS.
4. IT IS NOT THE INTENT OF THESE PLANS TO SHOW EVERY MINOR DETAIL OF CONSTRUCTION. CONTRACTOR IS REQUIRED TO FURNISH AND INSTALL ANY/ALL ITEMS FOR A COMPLETE AND FULLY FUNCTIONAL SYSTEM SUBJECT ONLY TO OWNER-SUPPLIED ITEMS. CONTRACTOR SHALL PROVIDE ANY/ALL REQUIREMENTS FOR THE EQUIPMENT TO BE PLACED IN PROPER WORKING ORDER.
5. PLANS ARE NOT TO BE SCALED. THESE PLANS ARE INTENDED TO BE A DIAGRAMMATIC OUTLINE ONLY UNLESS OTHERWISE NOTED. THE WORK SHALL INCLUDE FURNISHING MATERIALS, EQUIPMENT AND APPURTENANCES, AND LABOR NECESSARY TO EFFECT ALL INSTALLATIONS AS INDICATED ON THE DRAWINGS. UNLESS CONTRACTOR INSTALLED MATERIALS WILL INCLUDE THE FOLLOWING, UNLESS NOTED OTHERWISE:
 - A) TRANSMITTER
 - B) UHF ANTENNA AND MOUNTING BRACKETS, GPS ANTENNAS AND KU ANTENNAS
 - C) UHF COAX AND HANGERS
 - D) INTEGRATED LOAD CENTER
6. DIMENSIONS SHOWN ARE TO FINISH SURFACES UNLESS OTHERWISE NOTED. SPACING BETWEEN EQUIPMENT IS REQUIRED CLEARANCE. THEREFORE, IT IS CRITICAL TO FIELD VERIFY DIMENSIONS. SHOULD THERE BE ANY QUESTIONS REGARDING THE CONTRACT DOCUMENTS, EXISTING CONDITIONS AND/OR DESIGN INTENT, THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPORTING ANY DISCREPANCIES TO THE ATTENTION OF THE MOBILITE CM, IN WRITING, PRIOR TO THE COMMENCEMENT OF WORK.
7. DETAILS PROVIDED ARE FOR THE PURPOSE OF SHOWING DESIGN INTENT. MODIFICATIONS MAY BE REQUIRED TO SUIT JOB DIMENSIONS OR SITE CONDITIONS, AND SUCH MODIFICATIONS SHALL BE INCLUDED AS PART OF THE WORK.
8. CONTRACTOR SHALL PAY FOR APPLICABLE PERMITS, FEES, INSPECTIONS AND TESTING. CONTRACTOR IS TO OBTAIN PERMITS AND APPROVED SUBMITTALS PRIOR TO ORDERING MATERIALS AND THE COMMENCEMENT OF WORK.
9. THE TERM "PROVIDE" USED IN CONSTRUCTION DOCUMENTS AND SPECIFICATIONS, INDICATES THAT THE CONTRACTOR SHALL FURNISH AND INSTALL.
10. CONTRACTOR SHALL RECEIVE CLARIFICATION IN WRITING, AND SHALL RECEIVE IN WRITING AUTHORIZATION TO PROCEED BEFORE STARTING WORK ON ANY ITEMS NOT CLEARLY DEFINED OR IDENTIFIED BY THE CONTRACT DOCUMENTS.
11. CONTRACTOR SHALL SUPERVISE AND DIRECT THE WORK USING ACCEPTED INDUSTRY-STANDARD SKILLS AND ATTENTION. CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES AND PROCEDURES AND FOR COORDINATING ALL PORTIONS OF THE WORK UNDER CONTRACT, UNLESS OTHERWISE NOTED.
12. CONTRACTOR SHALL BE RESPONSIBLE FOR THE SAFETY OF THE WORK AREA, ADJACENT AREAS AND BUILDING OCCUPANTS THAT ARE LIKELY TO BE AFFECTED BY THE WORK UNDER THIS CONTRACT. WORK SHALL CONFORM TO ALL OSHA REQUIREMENTS.
13. CONTRACTOR SHALL COORDINATE THEIR WORK WITH THE MOBILITE CM AND SCHEDULE THEIR ACTIVITIES AND WORKING HOURS IN ACCORDANCE WITH THE REQUIREMENTS.
14. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING THEIR WORK WITH THE WORK OF OTHERS AS IT MAY RELATE TO RADIO EQUIPMENT, ANTENNAS AND ANY OTHER PORTIONS OF THE WORK.
15. CONTRACTOR SHALL INSTALL ALL EQUIPMENT AND MATERIALS IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS UNLESS SPECIFICALLY OTHERWISE INDICATED OR WHERE LOCAL CODES OR REGULATIONS TAKE PRECEDENCE.
16. CONTRACTOR SHALL MAKE NECESSARY PROVISIONS TO PROTECT EXISTING SURFACES, EQUIPMENT, IMPROVEMENTS, PIPING ETC. AND IMMEDIATE REPAIR, TO NEW CONDITION. ANY DAMAGE THAT OCCURS DURING CONSTRUCTION AT THE SOLE COST OF THE CONTRACTOR.

17. IN DRILLING HOLES, OR CORING, INTO CONCRETE WHETHER FOR FASTENING OR ANCHORING PURPOSES, OR PENETRATIONS THROUGH THE FLOOR FOR CONDUIT RUNS, PIPE RUNS, ETC., MUST BE CLEARLY UNDERSTOOD THAT REINFORCING STEEL ARE NOT DEFINITELY KNOWN AND THEREFORE MUST BE LOCATED BY THE CONTRACTOR USING APPROPRIATE METHODS AND EQUIPMENT PRIOR TO ANY DRILLING OR CORING OPERATIONS IN EXISTING CONCRETE. CONTRACTOR SHALL REPAIR, TO NEW CONDITION, ALL EXISTING WALL SURFACES DAMAGED DURING CONSTRUCTION SUCH THAT THEY MATCH AND BLEND IN WITH ADJACENT SURFACES.
18. CONTRACTOR SHALL SEAL PENETRATIONS THROUGH FIRE RATED ASSEMBLIES OR MATERIALS WITH U.L. LISTED AND FIRE CODE APPROVED MATERIALS AND SYSTEMS THAT MEET OR EXCEED THE RATING OF THE ASSEMBLY IN WHICH THE NEW PENETRATION IS PLACED.
19. CONTRACTOR SHALL KEEP CONTRACT AREA CLEAN, HAZARD FREE, AND DISPOSE OF ALL DIRT, DEBRIS, AND RUBBISH. EQUIPMENT NOT SPECIFIED AS REMAINING ON THE PROPERTY OF THE OWNER SHALL BE REMOVED. LEAVE PREMISES IN CLEAN CONDITION AND FREE FROM PAINT SPOTS, DUST, OR SMUDGES OF ANY NATURE. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL ITEMS UNTIL COMPLETION OF CONSTRUCTION.
20. MINIMUM BEND RADIUS OF ANTENNA CABLES SHALL BE IN ACCORDANCE WITH CABLE MANUFACTURERS RECOMMENDATIONS.
21. CONTRACTOR SHALL MINIMIZE DISTURBANCE TO EXISTING SITE DURING CONSTRUCTION. EROSION CONTROL MEASURES, DURING CONSTRUCTION SHALL BE IN CONFORMANCE WITH JURISDICTIONAL OR STATE AND LOCAL GUIDELINES FOR EROSION AND SEDIMENT CONTROL, AND COORDINATED WITH LOCAL REGULATORY AUTHORITIES. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE OF ANY EROSION CONTROL MEASURES, RECORD-KEEPING, MONITORING, AND REPORTING TO THE OWNER AND REGULATORY AUTHORITIES.
22. ALL CONSTRUCTION WORK IS TO ADHERE TO APPLICANT'S INTEGRATED CONSTRUCTION STANDARDS UNLESS STATE OR LOCAL CODE IS MORE STRINGENT.
23. THE INTENT OF THE PLANS AND SPECIFICATIONS IS TO PERFORM THE CONSTRUCTION IN ACCORDANCE PER STATE BUILDING STANDARDS CODE AND STATE CODE OF REGULATIONS. SHOULD ANY CONDITIONS DEVELOP NOT COVERED BY THE APPROVED PLANS AND SPECIFICATIONS WHEREIN THE FINISHED WORK WILL NOT COMPLY PER STATE CODE OF REGULATIONS, A SCOPE OF WORK DETAILING AND SPECIFYING THE REQUIRED WORK SHALL BE SUBMITTED TO AND APPROVED BY THE JURISDICTION BEFORE PROCEEDING WITH THE WORK. A CHANGE ORDER FOR THAT SCOPE SHALL BE SUBMITTED TO THE MOBILITE CM PRIOR TO PROCEEDING WITH THE WORK.
24. ADEQUATE AND REQUIRED LIABILITY INSURANCE SHALL BE PROVIDED BY THE CONTRACTOR FOR PROTECTION AGAINST PUBLIC LOSS AND ANY/ALL PROPERTY DAMAGE FOR THE DURATION OF WORK.
25. CONTRACTOR SHALL GUARANTEE ANY/ALL MATERIALS AND WORK FREE FROM DEFECTS FOR A PERIOD OF NOT LESS THAN ONE YEAR FROM DATE OF ACCEPTANCE. ANY CORRECTIVE WORK SHALL BE COMPLETED AT THE SOLE COST OF THE CONTRACTOR.

ELECTRICAL NOTES:

1. ELECTRICAL CONTRACTOR SHALL SUPPLY AND INSTALL ANY/ALL ELECTRICAL WORK INDICATED. ANY/ALL CONSTRUCTION SHALL BE IN ACCORDANCE W/DRAWINGS AND ANY/ALL APPLICABLE SPECIFICATIONS. IF ANY PROBLEMS ARE ENCOUNTERED BY COMPLYING WITH THESE REQUIREMENTS, CONTRACTOR SHALL NOTIFY MOBILITE CM AS SOON AS POSSIBLE AFTER THE DISCOVERY OF THE PROBLEMS, AND SHALL NOT PROCEED WITH THAT PORTION OF WORK, UNTIL THE MOBILITE CM HAS DIRECTED THE CORRECTIVE ACTIONS TO BE TAKEN.
2. ELECTRICAL CONTRACTOR SHALL VISIT THE JOB SITE AND FAMILIARIZE THEMSELVES WITH ANY/ALL CONDITIONS AFFECTING ELECTRICAL AND COMMUNICATION INSTALLATION AND MAKE PROVISIONS AS TO THE COST THEREOF. ALL EXISTING CONDITIONS OF ELECTRICAL EQUIP., ETC., THAT ARE PART OF THE FINAL SYSTEM, SHALL BE VERIFIED BY THE CONTRACTOR, PRIOR TO THE SUBMITTING OF THEIR BID. FAILURE TO COMPLY WITH THIS PARAGRAPH WILL IN NO WAY RELIEVE CONTRACTOR OF PERFORMING ALL WORK NECESSARY FOR A COMPLETE AND WORKING SYSTEM.
3. ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE LATEST EDITION OF THE NEC, ALL CODES AND ORDINANCES OF THE LOCAL JURISDICTION AND POWER & TELEPHONE COMPANIES HAVING JURISDICTION AND SHALL INCLUDE BUT ARE NOT BE LIMITED TO:
 - A) UL - UNDERWRITERS LABORATORIES
 - B) NEC - NATIONAL ELECTRICAL CODE
 - C) NEMA - NATIONAL ELECTRICAL MANUFACTURERS ASSOC.
 - D) OSHA - OCCUPATIONAL SAFETY AND HEALTH ACT
 - E) SBC - STANDARD BUILDING CODE
 - F) NFPA - NATIONAL FIRE PROTECTION AGENCY
 - G) ANSI - AMERICAN NATIONAL STANDARDS INSTITUTE
 - H) IEEE - INSTITUTE OF ELECTRICAL AND ELECTRONICS ENGINEERS
 - I) ASTM - AMERICAN SOCIETY FOR TESTING MATERIALS

4. REFER TO SITE PLANS AND ELEVATIONS FOR EXACT LOCATIONS OF ALL EQUIPMENT, AND CONFIRM WITH MOBILITE CM ANY SIZES AND LOCATIONS WHEN NEEDED.
5. EXISTING SERVICES: CONTRACTOR SHALL NOT INTERRUPT EXISTING SERVICES WITHOUT WRITTEN PERMISSION OF THE OWNER.
6. CONTRACTOR SHALL CONFIRM WITH LOCAL UTILITY COMPANY ANY/ALL REQUIREMENTS SUCH AS THE: LUG SIZE RESTRICTIONS, CONDUIT ENTRY, SIZE OF TRANSFORMERS, SCHEDULED DOWNTIME FOR THE OWNERS CONFIRMATION, ETC... ANY/ALL CONFLICTS SHALL BE BROUGHT TO THE ATTENTION OF THE MOBILITE CM, PRIOR TO BEGINNING ANY WORK.
7. MINIMUM WIRE SIZE SHALL BE #12 AWG, NOT INCLUDING CONTROL WIRING, UNLESS NOTED OTHERWISE. ALL CONDUCTORS SHALL BE COPPER WITH THIN INSULATION, UNLESS OTHERWISE NOTED.
8. OUTLET BOXES SHALL BE PRESSED STEEL IN DRY LOCATIONS, CAST ALLOY WITH THREADED HUBS IN WET/DAMP LOCATIONS AND SPECIAL ENCLOSURES FOR OTHER CLASSIFIED AREAS.
9. IT IS NOT THE INTENT OF THESE PLANS TO SHOW EVERY MINOR DETAIL OF THE CONSTRUCTION. CONTRACTOR IS EXPECTED TO FURNISH AND INSTALL ALL ITEMS FOR A COMPLETE ELECTRICAL SYSTEM AND PROVIDE ALL REQUIREMENTS FOR THE EQUIPMENT TO BE PLACED IN PROPER WORKING ORDER.
10. ELECTRICAL SYSTEM SHALL BE AS COMPLETELY AND EFFECTIVELY GROUNDED, AS REQUIRED BY SPECIFICATIONS, SET FORTH BY APPLICANT.
11. ALL WORK SHALL BE PERFORMED BY A LICENSED ELECTRICAL CONTRACTOR IN A FIRST CLASS, WORKMANLIKE MANNER. THE COMPLETED SYSTEM SHALL BE FULLY FUNCTIONAL AND SHALL BE APPROVED BY THE MOBILITE CM AND LOCAL JURISDICTION. ANY DEFICIENCIES SHALL BE CORRECTED BY AN ELECTRICAL CONTRACTOR AT THE SOLE COST OF THE CONTRACTOR.
12. ALL WORK SHALL BE COORDINATED WITH OTHER TRADES TO AVOID INTERFERENCE WITH THE PROGRESS OF CONSTRUCTION.



PROJECT NO: RV90XB08D
 DRAWN BY: MR
 CHECKED BY: JF

1	07/16/10	CLIENT COMMENTS
2	01/25/11	CONSTRUCTION DRAWINGS

IF IT IS A VIOLATION OF THE LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT

RV90XB08D
 RIVERSIDE, CA 92508
 (N) CONCRETE LIGHT POLE

SHEET TITLE
 GENERAL NOTES

SHEET NUMBER
 GN-1

ELECTRICAL NOTES CONT'D

13. THE CORRECTION OF ANY DEFECTS SHALL BE COMPLETED BY THE CONTRACTOR WITHOUT ANY ADDITIONAL CHARGE AND SHALL INCLUDE THE REPLACEMENT OR THE REPAIR OF ANY OTHER PHASE OF THE INSTALLATION, WHICH MAY HAVE BEEN DAMAGED THEREIN.
14. CONTRACTOR SHALL PROVIDE AND INSTALL CONDUIT, CONDUCTORS, PULL WIRES, BOXES, COVER PLATES AND DEVICES FOR ALL OUTLETS AS INDICATED.
15. DITCHING AND BACK FILL: CONTRACTOR SHALL PROVIDE FOR ALL UNDERGROUND INSTALLED CONDUIT AND/OR CABLES INCLUDING EXCAVATION AND BACKFILLING AND COMPACTION. REFER TO NOTES AND REQUIREMENTS 'EXCAVATION AND BACKFILLING'.
16. MATERIALS, PRODUCTS AND EQUIPMENT, INCLUDING ALL COMPONENTS THEREOF, SHALL BE NEW AND SHALL APPEAR ON THE LIST OF U.L. APPROVED ITEMS AND SHALL MEET OR EXCEED THE REQUIREMENTS OF THE NEC, NEMA AND ETC.
17. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS OR MANUFACTURER'S CATALOG INFORMATION OF ANY/ALL EQUIPMENT AND ALL OTHER ELECTRICAL ITEMS FOR APPROVAL BY THE MOBILITE CM PRIOR TO INSTALLATION.
18. ANY CUTTING OR PATCHING DEEMED NECESSARY FOR ELECTRICAL WORK IS THE ELECTRICAL CONTRACTOR'S RESPONSIBILITY AND SHALL BE INCLUDED IN THE COST FOR WORK AND PERFORMED TO THE SATISFACTION OF THE MOBILITE CM UPON FINAL ACCEPTANCE.
19. THE ELECTRICAL CONTRACTOR SHALL LABEL ALL PANELS WITH ONLY TYPED WRITTEN DIRECTORIES. ALL ELECTRICAL WIRING SHALL BE THE RESPONSIBILITY OF THE ELECTRICAL CONTRACTOR.
20. DISCONNECT SWITCHES SHALL BE UL-RATED, H.P. RATED HEAVY-DUTY, QUICK-MAKE AND QUICK-BREAK ENCLOSURES, AS REQUIRED BY EXPOSURE TYPE.
21. ALL CONNECTIONS SHALL BE MADE WITH A PROTECTIVE COATING OF AN ANTI-OXIDE COMPOUND KNOWN AS "190-OXIDE K" BY DEARBORN CHEMICAL CO. COAT ALL WIRE SURFACES BEFORE CONNECTING. EXPOSED COPPER SURFACES, INCLUDING GROUND BARS, SHALL BE TREATED - NO SUBSTITUTIONS.
22. RACEWAYS: CONDUIT SHALL BE SCHEDULE 80 PVC MEETING OR EXCEEDING NEMA TC2 - 1990. CONTRACTOR SHALL PLUG AND CAP EACH END OF SPARE AND EMPTY CONDUITS AND PROVIDE TWO SEPARATE PULL STRINGS - 200 LBS TEST POLYETHYLENE CORD. ALL CONDUIT BENDS SHALL BE A MINIMUM OF 2 FT. RADIUS. RIGID CONDUITS WHEN SPECIFIED SHALL MEET UL-8 FOR GALVANIZED STEEL. ALL FITTINGS SHALL BE SUITABLE FOR USE WITH THREADED RIGID CONDUIT. COAT ALL THREADS WITH 'BRITZ ZINC' OR 'COLD GALV'.
23. SUPPORT OF ALL ELECTRICAL WORK SHALL BE AS REQUIRED BY NEC.
24. CONDUCTORS: CONTRACTOR SHALL USE 98% CONDUCTIVITY COPPER WITH TYPE THIN INSULATION UNLESS OTHERWISE NOTED. 600 VOLT, COLOR CODED. USE SOLID CONDUCTORS FOR WIRE UP TO AND INCLUDING NO. 8 AWG. USE STRANDED CONDUCTORS FOR WIRE ABOVE NO. 8 AWG.
25. CONNECTORS FOR POWER CONDUCTORS: CONTRACTOR SHALL USE PRESSURE TYPE INSULATED TWIST-ON CONNECTORS FOR NO. 10 AWG AND SMALLER. USE SOLDERLESS MECHANICAL TERMINAL LUGS FOR NO. 8 AWG AND LARGER.
26. SERVICE AS SPECIFIED ON THE DRAWINGS. OWNER OR OWNER'S AGENT WILL APPLY FOR POWER. ALL PROVISIONS FOR TEMPORARY POWER WILL BE OBTAINED BY THE CONTRACTOR.
27. TELEPHONE OR FIBER SERVICE: CONTRACTOR SHALL PROVIDE EMPTY CONDUITS WITH PULL STRINGS AS INDICATED ON DRAWINGS.
28. ELECTRICAL AND TELCO/FIBER RACEWAYS TO BE BURIED A MINIMUM DEPTH OF 30", UNLESS OTHERWISE NOTED.
29. CONTRACTOR SHALL PLACE 6" WIDE DETECTABLE WARNING TAPE AT A DEPTH OF 6" BELOW GROUND AND DIRECTLY ABOVE ELECTRICAL AND TELCO SERVICE CONDUITS. CAUTIONS TAPE TO READ "CAUTION BURIED ELECTRIC" OR "BURIED TELECOM".
30. ALL BOLTS SHALL BE 3-16 STAINLESS STEEL.

GROUNDING NOTES:

1. ALL HARDWARE SHALL BE 3-16 STAINLESS STEEL, INCLUDING LOCK WASHERS. COAT ALL SURFACES WITH AN ANTI-OXIDANT COMPOUND, AS SPECIFIED, BEFORE MATING. ALL HARDWARE SHALL BE STAINLESS STEEL 3/8 INCH DIAMETER OR LARGER.
2. FOR GROUND BOND TO STEEL ONLY: INSERT A CADMIUM FLAT WASHER BETWEEN LUG AND STEEL. COAT ALL SURFACES WITH AN ANTI-OXIDANT COMPOUND BEFORE MATING.
3. ALL STEEL CONDUIT SHALL BE BONDED AT BOTH ENDS WITH GROUNDING BUSHING.
4. ALL ELECTRICAL AND GROUNDING AT THE POLE SITE SHALL COMPLY WITH THE NATIONAL ELECTRICAL CODE (NEC), NATIONAL FIRE PROTECTION ASSOCIATION (NFPA) 780 (LATEST EDITION), AND MANUFACTURER.
5. ALL DETAILS ARE SHOWN IN GENERAL TERMS. ACTUAL GROUNDING INSTALLATION AND CONSTRUCTION MAY VARY DUE TO SITE SPECIFIC CONDITIONS.

6. GROUND ALL ANTENNA BASES, FRAMES, CABLE RUNS, AND OTHER METALLIC COMPONENTS USING #8 GROUND WIRES. FOLLOW ANTENNA AND BTS MANUFACTURER'S PRACTICES FOR GROUNDING REQUIREMENTS.
7. ALL GROUND CONNECTIONS SHALL BE #8 AWG UNLESS OTHERWISE NOTED. ALL WIRES SHALL BE COPPER WITH THIN INSULATION UNLESS OTHERWISE NOTED. ALL GROUND WIRE SHALL BE SOLID TIN COATED OR STRANDED GREEN INSULATED WIRE.
8. CONTRACTOR TO VERIFY AND TEST GROUND TO SOURCE, 10 OHMS MAXIMUM. PROVIDE SUPPLEMENT GROUNDING ROOFS AS REQUIRED TO ACHIEVE SPECIFIED OHMS READING. GROUNDING AND OTHER OPTIONAL TESTING WILL BE WITNESSED BY THE MOBILITE CM.
9. NOTIFY ARCHITECT/ENGINEER IF THERE ARE ANY DIFFICULTIES INSTALLING GROUNDING SYSTEM DUE TO SITE SOIL CONDITIONS.
10. ALL HORIZONTALLY RUN GROUNDING CONDUCTORS SHALL BE INSTALLED A MINIMUM OF 30" BELOW GRADE/ 6" BELOW FROST-LINE IN TRENCH, UNLESS OTHERWISE NOTED. BACK FILL SHALL BE COMPACTED AS REQUIRED BY ARCHITECT/ENGINEER.
11. ALL GROUND CONDUCTORS SHALL BE RUN AS STRAIGHT AND SHORT AS POSSIBLE, WITH A MINIMUM 12" BENDING RADIUS NOT LESS THAN 90 DEGREES.
12. ACCEPTABLE CONNECTIONS FOR GROUNDING SYSTEM SHALL BE:
12.A. RNDY, HY-GRADE U.L. LISTED CONNECTORS FOR OUTDOOR USE OR AS APPROVED BY APPLICANT PROJECT MANAGER. COLDWELDED, EXOTHERMIC WELDS WELDED CONNECTIONS.
12.B. ONE (1) HOLE TINNED COPPER COMPRESSION (LONG BARREL) FITTINGS.
13. ALL CRIMPED CONNECTIONS SHALL HAVE EMBOSSED MANUFACTURER'S DIE MARK VISIBLE AT THE CRIMP (RESULTING FROM USE OF PROPER CRIMPING DEVICES) AND WEATHER-PROOFED WITH HEAT SHRINK.
14. ALL CONNECTION HARDWARE SHALL BE TYPE 3-16 STAINLESS STEEL (NOT ATTRACTED TO MAGNETS). ELECTRICAL SERVICE EQUIPMENT GROUNDING SHALL COMPLY WITH NEC, ARTICLE 250-82 AND SHALL BOND ALL (E) AND NEW GROUNDING ELECTRODES. NEW GROUNDING ELECTRODE SHALL INCLUDE BUT NOT LIMITED TO GROUND RODS.

TESTING AND EQUIPMENT TURN UP REQUIREMENTS:

1. RF CABLE, DATA CABLE, RADIO EQUIPMENT AND BACK HAUL EQUIPMENT TESTING WILL COMPLY WITH CURRENT INDUSTRY STANDARDS AND OR THOSE STANDARDS OF THE EQUIPMENT MANUFACTURER OR PROVIDED TO THE CONTRACTOR PRIOR TO TESTING.
2. CONTRACTOR WILL USE THE APPROPRIATE CALIBRATED TESTING EQUIPMENT IN THE TESTING OF RF CABLE, DATA CABLE, RADIO EQUIPMENT AND BACK HAUL EQUIPMENT THAT MEET INDUSTRY STANDARDS OF THE MANUFACTURER OR THOSE STANDARDS PROVIDED TO THE CONTRACTOR PRIOR TO TESTING.
3. CONTRACTOR TO VERIFY AND RECORD ALL TEST RESULTS AND PROVIDE THESE RESULTS WITHIN THE FINAL CLOSE OUT PACKAGE.
4. ALL PERSONNEL INVOLVED IN THE TESTING OF RF CABLE, DATA CABLE, RADIO EQUIPMENT AND BACK HAUL EQUIPMENT WILL BE REQUIRED TO HAVE BEEN TRAINED AND OR CERTIFIED IN THE PROPER TESTING OF RF CABLE, DATA CABLE, RADIO EQUIPMENT AND BACK HAUL EQUIPMENT.
5. ALL TEST RESULTS SHALL BE TIME STAMPED, RECORDED AND PRESENTED PRIOR TO ENERGIZING AND TURN UP OF ANY EQUIPMENT.
6. GPS EQUIPMENT IS NOT TO BE TESTED OR ATTACHED TO ANY CABLE DURING TESTING, DOING SO WILL DAMAGE THE GPS UNIT.
7. PRIOR TO TESTING IF THE CONTRACTOR HAS ANY QUESTIONS ABOUT THE TESTING PROCEDURES THEY ARE TO CALL AND OBTAIN ASSISTANCE FROM A QUALIFIED DESIGNATED TESTING REPRESENTATIVE.
8. EQUIPMENT IS NOT TO BE ENERGIZED UNTIL ALL TESTING HAS BEEN COMPLETED, APPROVED AND THE APPROPRIATE AUTHORITY HAS BEEN NOTIFIED AND GIVES APPROVAL TO ENERGIZE THE EQUIPMENT.

SITE WORK NOTES:

1. DO NOT EXCAVATE OR DISTURB BEYOND THE PROPERTY LINES OR LEASE LINES, UNLESS OTHERWISE NOTED.
2. SIZE, LOCATION AND TYPE OF ANY UNDERGROUND UTILITIES OR IMPROVEMENTS SHALL BE ACCURATELY NOTED AND PLACED ON AS-BUILT DRAWINGS BY GENERAL CONTRACTOR AND ISSUED TO ARCHITECT/ENGINEER AT COMPLETION OF PROJECT.
3. ALL (E) UTILITIES, FACILITIES, CONDITIONS AND THEIR DIMENSIONS SHOWN ON PLANS HAVE BEEN PLOTTED FROM AVAILABLE RECORDS. THE ENGINEER AND OWNER ASSUME NO RESPONSIBILITY WHATSOEVER AS TO THE SUFFICIENCY OR ACCURACY OF THE INFORMATION SHOWN ON THE PLANS OR THE MANNER OF THEIR REMOVAL OR ADJUSTMENT. CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING EXACT LOCATION OF ALL (E) UTILITIES AND FACILITIES PRIOR TO START OF CONSTRUCTION. CONTRACTOR SHALL ALSO OBTAIN FROM EACH UTILITY COMPANY DETAILED INFORMATION RELATIVE TO WORKING SCHEDULES AND METHODS OF REMOVING OR ADJUSTING (E) UTILITIES.

4. CONTRACTOR SHALL VERIFY ALL (E) UTILITIES BOTH HORIZONTALLY AND VERTICALLY PRIOR TO START OF CONSTRUCTION. ANY DISCREPANCIES OR DOUBTS AS TO THE INTERPRETATION OF PLANS SHALL BE IMMEDIATELY REPORTED TO THE ARCHITECT/ENGINEER OR MOBILITE CM FOR RESOLUTION AND INSTRUCTION. AND NO FURTHER WORK SHALL BE PERFORMED UNTIL THE DISCREPANCY IS CHECKED AND CORRECTED BY THE ARCHITECT/ENGINEER. FAILURE TO SECURE SUCH INSTRUCTION MEANS CONTRACTOR WILL HAVE WORKED AT THEIR OWN RISK AND EXPENSE. CONTRACTOR SHALL CALL LOCAL UTILITY LOCATE HOT LINE, SUCH AS 811, FOR UTILITY LOCATIONS A MINIMUM OF 48 HOURS PRIOR TO START OF CONSTRUCTION.

5. ALL NEW AND (E) UTILITY STRUCTURES ON SITE AND IN AREAS TO BE DISTURBED BY CONSTRUCTION SHALL BE ADJUSTED TO FINISH ELEVATIONS PRIOR TO FINAL INSPECTION OF WORK. ANY COST RELATED TO ADJUSTING (E) STRUCTURES SHALL BE BORNE SOLELY BY THE CONTRACTOR.

6. GRADING OF THE SITE WORK AREA IS TO BE SMOOTH AND CONTINUOUS IN SLOPE AND IS TO FEATHER INTO (E) GRADES AT THE GRADING LIMITS.

7. ALL TEMPORARY EXCAVATIONS FOR THE INSTALLATION OF FOUNDATIONS, UTILITIES, ETC., SHALL BE PROPERLY LAID BACK OR BRACED IN ACCORDANCE WITH CORRECT OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) REQUIREMENTS.



PROJECT NO: RV80XSB00
 DRAWN BY: MR
 CHECKED BY: JF

1 07/18/10 CLIENT COMMENTS
 0 07/26/10 CONSTRUCTION DRAWINGS

IT IS A VIOLATION OF THE LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT

RV80XSB000
 RIVERSIDE, CA 92508
 (N) CONCRETE LIGHT POLE

SHEET TITLE
 GENERAL NOTES

SHEET NUMBER
 GN-2

SITE WORK NOTES CONT'D

8. STRUCTURAL FILLS SUPPORTING PAVEMENTS SHALL BE COMPACTED TO 95% OF MAXIMUM STANDARD PROCTOR DRY DENSITY, UNLESS OTHERWISE NOTED.
9. NEW GRADES NOT IN BUILDING AND DRIVEWAY IMPROVEMENT AREA TO BE ACHIEVED BY FILLING WITH APPROVED CLEAN FILL AND COMPACTED TO 95% OF STANDARD PROCTOR DENSITY.
10. ALL FILL SHALL BE PLACED IN UNIFORM LIFTS. THE LIFTS THICKNESS SHOULD NOT EXCEED THAT WHICH CAN BE PROPERLY COMPACTED THROUGHOUT ITS ENTIRE DEPTH WITH THE EQUIPMENT AVAILABLE.
11. ANY FILLS PLACED ON (E) SLOPES THAT ARE STEEPER THAN 10 HORIZONTAL TO 1 VERTICAL SHALL BE PROPERLY BENCHED INTO THE (E) SLOPE AS DIRECTED BY A GEOTECHNICAL ENGINEER.
12. CONTRACTOR SHALL CLEAN ENTIRE SITE AFTER CONSTRUCTION SUCH THAT NO DEBRIS, PAPER, TRASH, WEEDS, BRUSH, EXCESS FILL, OR ANY OTHER DEPOSITS WILL REMAIN. ALL MATERIALS COLLECTED DURING CLEANING OPERATIONS SHALL BE DISPOSED OF OFF-SITE BY THE GENERAL CONTRACTOR.
13. ALL TREES AND SHRUBS WHICH ARE NOT IN DIRECT CONFLICT WITH THE IMPROVEMENTS SHALL BE PROTECTED BY THE GENERAL CONTRACTOR.
14. ALL SITE WORK SHALL BE CAREFULLY COORDINATED BY GENERAL CONTRACTOR WITH LOCAL UTILITY COMPANY, TELEPHONE COMPANY, AND ANY OTHER UTILITY COMPANIES HAVING JURISDICTION OVER THIS LOCATION.

ENVIRONMENTAL NOTES

1. ALL WORK PERFORMED SHALL BE DONE IN ACCORDANCE WITH ISSUED PERMITS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF FINES AND PROPER CLEAN UP FOR AREAS IN VIOLATION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR CONSTRUCTION AND MAINTENANCE OF EROSION AND SEDIMENTATION CONTROLS DURING CONSTRUCTION FOR PROTECTION OF ADJACENT PROPERTIES, ROADWAYS AND WATERWAYS. ALL EROSION AND SEDIMENTATION CONTROLS SHALL BE MAINTAINED IN PLACE THROUGH FINAL JURISDICTIONAL INSPECTION & RELEASE OF SITE.
3. CONTRACTOR SHALL INSTALL/CONSTRUCT ALL NECESSARY SEDIMENT/SILT CONTROL FENCING AND PROTECTIVE MEASURES AS REQUIRED BY THE LOCAL JURISDICTION WITHIN THE LIMITS OF SITE DISTURBANCE PRIOR TO CONSTRUCTION.
4. NO SEDIMENT SHALL BE ALLOWED TO EXIT THE PROPERTY. THE CONTRACTOR IS RESPONSIBLE FOR TAKING ADEQUATE MEASURES FOR CONTROLLING EROSION. ADDITIONAL SEDIMENT CONTROL FENCING MAY BE REQUIRED IN ANY AREAS SUBJECT TO EROSION.
5. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING POSITIVE DRAINAGE ON THE SITE AT ALL TIMES WITH SILT AND EROSION CONTROL MEASURES MAINTAINED ON THE DOWNSTREAM SIDE OF SITE DRAINAGE. ANY DAMAGE TO ADJACENT PROPERTY AS A RESULT OF EROSION WILL BE CORRECTED AT THE CONTRACTORS EXPENSE.
6. CONTRACTOR SHALL BE RESPONSIBLE FOR DAILY INSPECTIONS AND ANY REPAIRS OF ALL SEDIMENT CONTROL MEASURES INCLUDING SEDIMENT REMOVAL AS NECESSARY.
7. CLEARING OF VEGETATION AND TREE REMOVAL SHALL BE ONLY AS PERMITTED AND BE HELD TO A MINIMUM. ONLY TREES NECESSARY FOR CONSTRUCTION OF THE FACILITIES SHALL BE REMOVED.
8. SEEDING AND MULCHING AND/OR SOODING OF THE SITE WILL BE ACCOMPLISHED AS SOON AS POSSIBLE AFTER COMPLETION OF THE PROJECT FACILITIES AFFECTING LAND DISTURBANCE.
9. CONTRACTOR SHALL PROVIDE ALL EROSION AND SEDIMENTATION CONTROL MEASURES AS REQUIRED BY LOCAL, COUNTY AND STATE CODES AND ORDINANCES TO PROTECT EMBANKMENTS FROM SOIL LOSS AND TO PREVENT ACCUMULATION OF SOIL AND SILT IN STREAMS AND DRAINAGE PATHS LEAVING THE CONSTRUCTION AREA. THIS MAY INCLUDE, BUT IS NOT LIMITED TO SUCH MEASURES AS SILT FENCES, STRAW BALE SEDIMENT BARRIERS, AND CHECK DAMS.
10. RIP RAP OF SIZES INDICATED SHALL CONSIST OF CLEAN, HARD, SOUND, DURABLE, UNIFORM IN QUALITY STONE FREE OF ANY DETRIMENTAL QUANTITY OF SOFT, FRAGILE, THIN, ELONGATED OR LAMINATED PIECES, DISINTEGRATED MATERIAL, ORGANIC MATTER, OIL, ALKALI OR OTHER DELETERIOUS SUBSTANCES.

11. GO TO PLACE FILTER MATERIAL AT ALL CATCH BASINS ADJACENT TO CONSTRUCTION SITE TO PREVENT SOLID WASTE CONTAMINATION FROM ENTERING SEWER SYSTEM FOUNDATION, EXCAVATION AND BACKFILL NOTES.

1. ALL FINAL GRADED SLOPES SHALL BE A MAXIMUM OF 3 HORIZONTAL TO 1 VERTICAL UNLESS OTHERWISE NOTED.
2. BACKFILL OF POLE SHALL BE PERFORMED IN ONE OF THREE OPTIONS:
 - A. PREFERRED: RAINBOW INDUSTRIES POLE SETTING FOAM SHALL BE INSTALLED PER MANUFACTURER SPECS. FOAM SHALL ALWAYS BE USED FOR POOR SOILS.
 - B. SECONDARY: CONCRETE (REQUIRES MOBILITIE CM WRITTEN APPROVAL) ALLOWABLE SOIL PRESSURE = 2000 PSF (ASSUMED). NON-NATIVE SOILS SHALL BE REMOVE FROM BORE AREA AND SHALL NOT BE REUSED FOR BACKFILL.
3. ALL EXCAVATIONS PREPARED FOR PLACEMENT OF CONCRETE SHALL BE OF UNDISTURBED SOILS, SUBSTANTIALLY HORIZONTAL AND FREE FROM ANY LOOSE, UNSUITABLE MATERIAL OR FROZEN SOILS AND WITHOUT THE PRESENCE OF POUNDING WATER. Dewatering FOR EXCESS GROUND WATER SHALL BE PROVIDED WHEN REQUIRED. COMPACTION OF SOILS UNDER CONCRETE PAD FOUNDATIONS SHALL NOT BE LESS THAN 95% OF THE MODIFIED PROCTOR MAXIMUM DRY DENSITY FOR THE SOIL IN ACCORDANCE WITH ASTM D1557.
4. CONCRETE FOUNDATIONS SHALL NOT BE PLACED ON ORGANIC OR UNSUITABLE MATERIAL. IF ADEQUATE BEARING CAPACITY IS NOT ACHIEVED AT THE DESIGNED EXCAVATION DEPTH THE UNSATISFACTORY SOIL SHALL BE EXCAVATED TO ITS FULL DEPTH AND EITHER BE REPLACED WITH MECHANICALLY COMPACTED GRANULAR MATERIAL OR THE EXCAVATION SHALL BE FILLED WITH CONCRETE OF THE SAME TYPE SPECIFIED FOR THE FOUNDATION. CRUSHED STONE MAY BE USED TO STABILIZE THE BOTTOM OF THE EXCAVATION. ANY STONE SUB BASE MATERIAL, IF USED, SHALL NOT SUBSTITUTE FOR REQUIRED THICKNESS OF CONCRETE.
5. ALL EXCAVATIONS SHALL BE CLEAN OF UNSUITABLE MATERIAL SUCH AS VEGETATION, TRASH, DEBRIS AND SO FORTH PRIOR TO BACK FILLING. BACK FILL SHALL CONSIST OF APPROVED MATERIALS SUCH AS EARTH, LOAM, SANDY CLAY, SAND AND GRAVEL, OR SOFT SHALE, FREE FROM CLODS OR LARGE STONES OVER 2 1/2" MAX DIMENSIONS. ALL BACK FILL SHALL BE PLACED IN COMPACTED LAYERS.
6. ALL FILL MATERIALS AND FOUNDATION BACK FILL SHALL BE PLACED IN MAXIMUM 6" THICK LIFTS BEFORE COMPACTION. EACH LIFT SHALL BE WETTED IF REQUIRED AND COMPACTED TO NOT LESS THAN 95% OF THE MODIFIED PROCTOR MAXIMUM DRY DENSITY FOR SOIL IN ACCORDANCE WITH ASTM D1557.
7. NEWLY PLACED CONCRETE FOUNDATIONS SHALL CURE A MINIMUM OF 72 HRS PRIOR TO BACK FILLING.
8. FINISHED GRADING SHALL BE SLOPED TO PROVIDE POSITIVE DRAINAGE AND PREVENT STANDING WATER. THE FINAL (FINISH) ELEVATION OF SLAB FOUNDATIONS SHALL SLOPE AWAY IN ALL DIRECTIONS FROM THE CENTER. FINISH GRADE OF CONCRETE PADS SHALL BE A MAXIMUM OF 4 INCHES ABOVE FINAL FINISH GRADE ELEVATIONS. PROVIDE SURFACE FILL GRAVEL TO ESTABLISH SPECIFIED ELEVATIONS WHERE REQUIRED.
9. NEWLY GRADED GRAVEL SURFACE AREAS TO RECEIVE GRAVEL SHALL BE COVERED WITH GEOTEXTILE FABRIC TYPE TYRAN-3400 AS MANUFACTURED BY TYRAN GEOSYNTHETICS OR AN APPROVED EQUIVALENT. SHOWN ON PLANS. THE GEOTEXTILE FABRIC SHALL BE BLACK IN COLOR TO CONTROL THE RECURRENT OF VEGETATIVE GROWTH AND EXTEND TO WITHIN 1 FOOT OUTSIDE THE SITE FENCING OR ELECTRICAL GROUNDING SYSTEM PERIMETER WHICH EVER IS GREATER. ALL FABRIC SHALL BE COVERED WITH A MINIMUM OF 4" DEEP COMPACTED STONE OR GRAVEL AS SPECIFIED. I.E. FDOT TYPE NO. 57 FOR FENCED COMPOUND; FDOT TYPE NO. 67 FOR ACCESS DRIVE AREA, UNLESS OTHERWISE NOTED.
10. IN ALL AREAS TO RECEIVE FILL: REMOVE ALL VEGETATION, TOPSOIL, DEBRIS, WET AND UNSATISFACTORY SOIL MATERIALS, OBSTRUCTIONS, AND DELETERIOUS MATERIALS FROM GROUND SURFACE. PLOW STRIP OR BREAK UP SLOPED SURFACES STEEPER THAN 1 VERTICAL TO 4 HORIZONTAL SUCH THAT FILL MATERIAL WILL BIND WITH (E)/PREPARED SOIL SURFACE.
11. WHEN SUB GRADE OR PREPARED GROUND SURFACE HAS A DENSITY LESS THAN THAT REQUIRED FOR THE FILL MATERIAL, SCARP THE GROUND SURFACE TO DEPTH REQUIRED. PULVERIZE, MOISTURE-CONDITION AND/OR AERATE THE SOILS AND RECOMPACT TO THE REQUIRED DENSITY PRIOR TO PLACEMENT OF FILLS.
12. IN AREAS WHICH (E) GRAVEL SURFACING IS REMOVED OR DISTURBED DURING CONSTRUCTION OPERATIONS, REPLACE GRAVEL SURFACING TO MATCH ADJACENT GRAVEL SURFACING AND RESTORED TO THE SAME THICKNESS AND COMPACTION AS SPECIFIED. ALL RESTORED GRAVEL SURFACING SHALL BE FREE FROM CORRUGATIONS AND WAVES.
13. (E) GRAVEL SURFACING MAY NOT BE REUSED.

14. GRAVEL SUB SURFACE SHALL BE PREPARED TO REQUIRED COMPACTION AND SUB GRADE ELEVATIONS BEFORE GRAVEL SURFACING IS PLACED AND/OR RESTORED. ANY LOOSE OR DISTURBED MATERIALS SHALL BE THOROUGHLY COMPACTED AND ANY DEPRESSIONS IN THE SUB GRADE SHALL BE FILLED AND COMPACTED WITH APPROVED SELECTED MATERIAL. GRAVEL SURFACING MATERIAL SHALL NOT BE USED FOR FILLING DEPRESSIONS IN THE SUB GRADE.
15. PROTECT (E) GRAVEL SURFACING AND SUB GRADE IN AREAS WHERE EQUIPMENT LOADS WILL OPERATE. USE PLANKING "MATS" OR OTHER SUITABLE PROTECTION DESIGNED TO SPREAD EQUIPMENT LOADS AS MAY BE NECESSARY. REPAIR ANY DAMAGE TO (E) GRAVEL SURFACING OR SUB GRADE WHERE SUCH DAMAGE IS DUE TO THE CONTRACTORS OPERATIONS.
16. DAMAGE TO (E) STRUCTURES AND/OR UTILITIES RESULTING FROM CONTRACTORS NEGLIGENCE SHALL BE REPAIRED AND/ OR REPLACED TO THE OWNERS SATISFACTION AT NO ADDITIONAL COST TO THE CONTRACT.
17. ALL SUITABLE BORROW MATERIAL FOR BACK FILL OF THE SITE SHALL BE INCLUDED IN THE BID. EXCESS TOPSOIL AND UNSUITABLE MATERIAL SHALL BE DISPOSED OF OFF SITE AT LOCATIONS APPROVED BY GOVERNING AGENCIES AT NO ADDITIONAL COST TO THE CONTRACT.

OVERSIDE CITY NOTES

1. ALL FACILITIES INSTALLED BY DEVELOPER SHALL BE INSPECTED AND APPROVED BY THE THE DEPARTMENT CONSTRUCTION INSPECTOR (951-826-2416). ANY CHANGES SHALL BE APPROVED BY THE DEPARTMENT PRIOR TO EXECUTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL PHASES OF CONSTRUCTION WITH THE DEPARTMENT CONSTRUCTION INSPECTOR. THE DEPARTMENT INSPECTOR SHALL BE CONTACTED TWO WORKING DAYS IN ADVANCE OF ANY CONSTRUCTION INSPECTION TO ARRANGE FOR INSPECTION TIMES.
2. PROVIDE AND INSTALL ALL STREET LIGHT FACILITIES SHOWN INCLUDING STREET LIGHT CABLE, IN CONFORMANCE WITH THE FOLLOWING STANDARD DRAWINGS: UGS-100, UGS-800, 801 & STREET LIGHT SPECIFICATION #2-8.
3. INSTALL A SINGLE WATERPROOF "IN-LINE" FUSE HOLDER ON EACH PHASE LEG CONDUCTOR IN THE BASE OF EACH STREET LIGHT STANDARD. FUSE HOLDER TO BE "BUSSMAN TRON" #18B COMPATIBLE WITH SIZE AND TYPE OF WIRE WITH A "BUSSMAN" FNM 10 AMP 250V FUSE OR APPROVED EQUAL.
4. STREET LIGHT SERVICE POINTS ARE 3 WIRE, 240V SYSTEM. THE THIRD STREET LIGHT WIRE IS TO BE USED AS A BOND WIRE ONLY, AS NOTED IN SPECIFICATION #2-6. FEED POINTS TO BE CONFIRMED AT THE TIME OF CONSTRUCTION.
5. ALL CABLE TO BE AWG. #8 COPPER AS SPECIFIED IN STREET LIGHT SPECIFICATION #2-5 (STREET LIGHT CABLE).
6. SIZE AND TYPE OF WIRE SHALL BE AS SHOWN ON THE PLANS. COPPER WIRE CONDUCTORS SHALL CONFORM TO THE APPLICABLE PORTIONS OF ASTM B-3 AND B-8. ALUMINUM WIRE CONDUCTORS SHALL CONFORM TO THE APPLICABLE PORTIONS OF ASTM B-230, B-231, B-202 AND B323. WIRE FOR MULTIPLE STREET LIGHTING SYSTEMS SHALL BE AWG #8 CU IN THE LIGHT STANDARD AND IN THE RIGS RATED FOR 600 VOLT OPERATION AND SHALL HAVE EITHER 756C, THIN, 4/64 INCH, BLACK POLYVINYL CHLORIDE INSULATION OR 756C, THIN, 3/64 INCH, BLACK CROSS LINKED POLYETHYLENE INSULATION CONFORMING TO I.C.E.A. & NEMA STANDARDS. WIRE FOR 120V SYSTEM SHALL CONSIST ONE BLACK ONE RED AND ONE GREEN AWG #8 CU.
7. SIZE AND TYPE OF PULLBOXES SHALL BE AS SHOWN ON THE PLANS. PULLBOX COVERS SHALL BE INScribed "TRAFFIC SIGNALS" FOR TRAFFIC SIGNAL SYSTEMS OR COMBINED TRAFFIC SIGNAL AND STREET LIGHT SYSTEMS LESS THAN 600 VOLTS, AND "STREET LIGHTING" FOR STREET LIGHT SYSTEMS LESS THAN 600 VOLTS.
8. INSTALL A WATERPROOF "IN-LINE" FUSE HOLDER ON THE PHASE LEG CONDUCTOR(S) IN THE BASE OF EACH STREET LIGHT STANDARD. FUSE HOLDER TO BE "BUSSMAN TRON" #18B COMPATIBLE WITH SIZE AND TYPE OF WIRE, WITH A "BUSSMAN" FNM 10 AMP, 250V FUSE OR APPROVED EQUAL.
9. CONDUCTORS IN PULL BOXES SHALL BE SPLICED TOGETHER USING APPROVAL WIRE MOLDS.



PROJECT NO: RV90XS86D
 DRAWN BY: MR
 CHECKED BY: JF

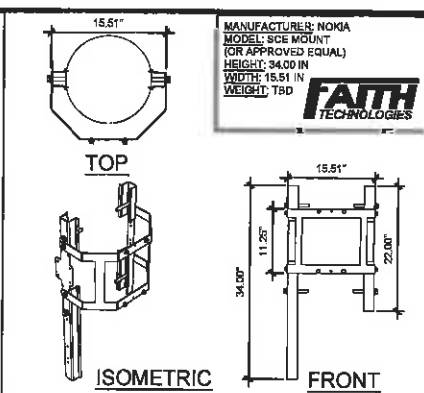
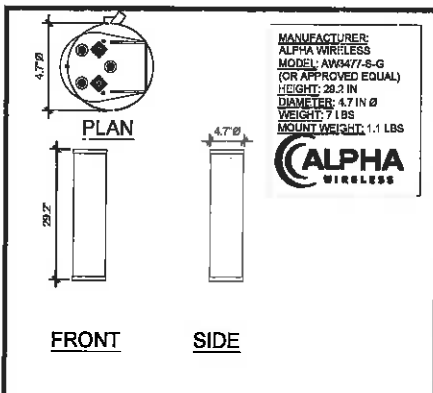
1	07/15/18	CLIENT COMMENTS
2		
3		
4		
5		
6		
7		
8		
9	07/25/18	CONSTRUCTION DRAWING

IF IS A VIOLATION OF THE LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT

RV90XS86D
 RIVERSIDE, CA 92508
 (N) CONCRETE LIGHT POLE

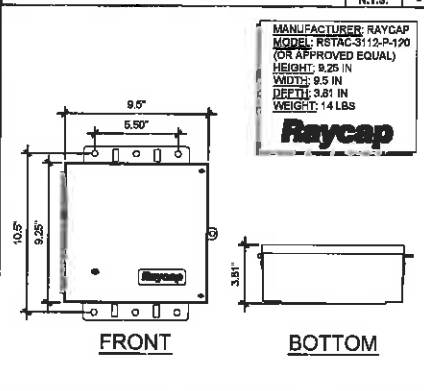
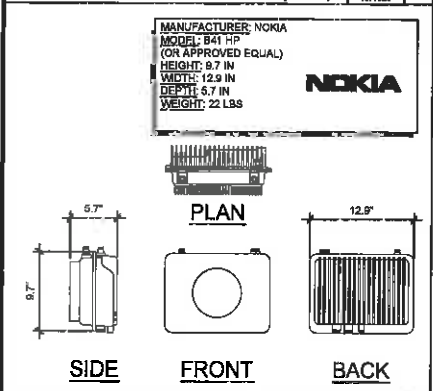
SHEET TITLE
 GENERAL NOTES

SHEET NUMBER
 GN-3



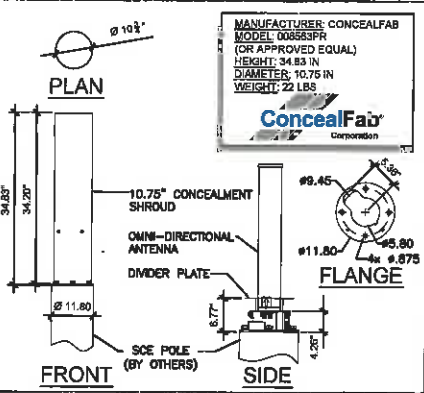
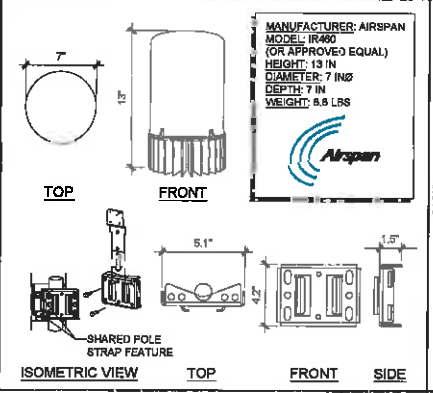
ALPHA AW3477-S-G OMNI (B41) SCALE N.T.S. 1

MOUNTING BRACKET SCALE N.T.S. 4



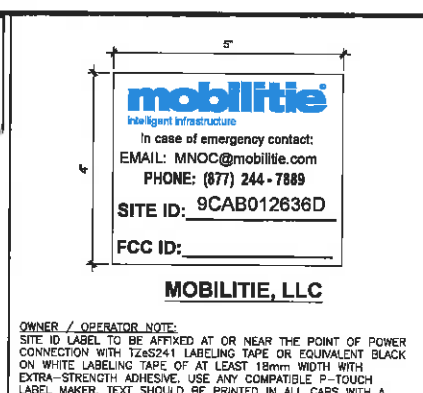
REMOTE RADIO HEAD SCALE N.T.S. 2

RAYCAP RSTAC-3112-P-120 SCALE N.T.S. 5

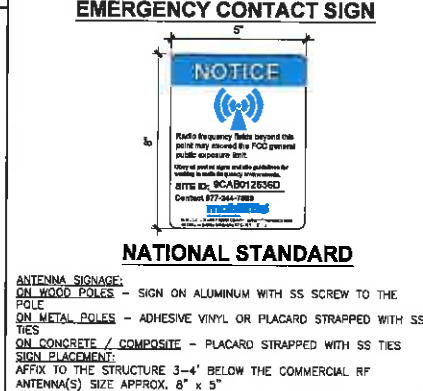


IR460 UE RELAY SCALE N.T.S. 3

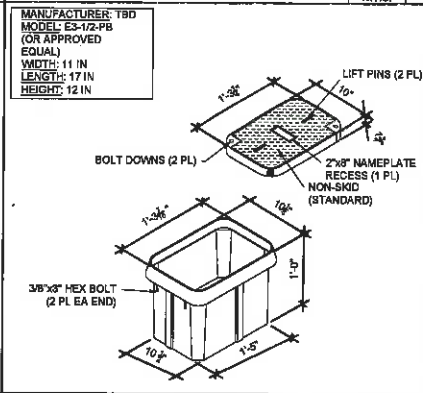
ANTENNA MOUNT W/ SHROUD SCALE N.T.S. 6



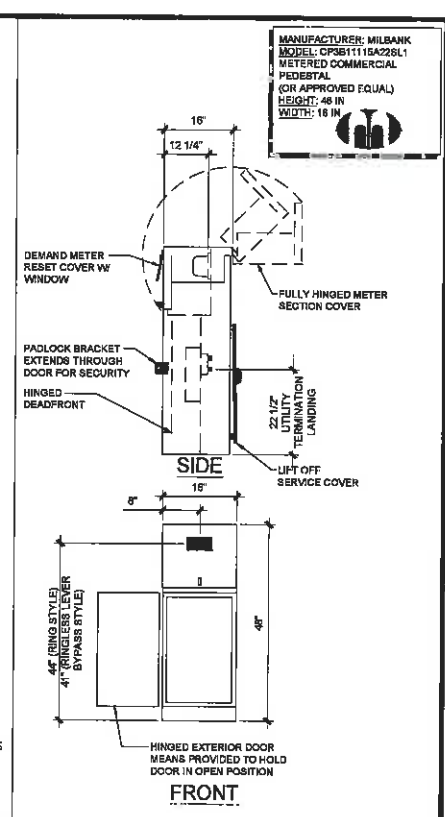
EMERGENCY CONTACT SIGN SCALE N.T.S. 4



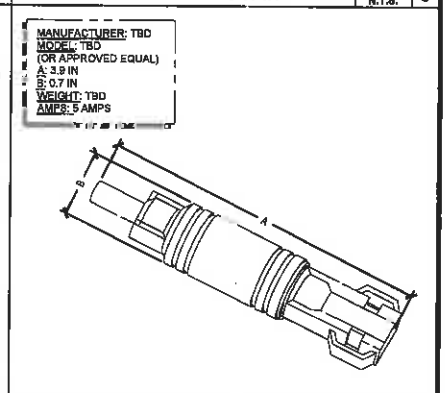
NATIONAL STANDARD SCALE N.T.S. 7



ELECTRICAL SERVICE BOX SCALE N.T.S. 8



COMMERCIAL PEDESTAL SCALE N.T.S. 9



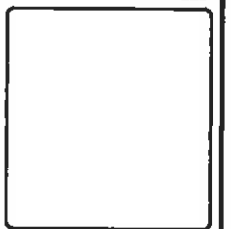
IN-LINE FUSE SCALE N.T.S. 10



PROJECT NO: RV80XSB8D
DRAWN BY: MR
CHECKED BY: JF

1	07/16/19	CLIENT COMMENTS
2	10/25/19	CONSTRUCTION DRAWINGS

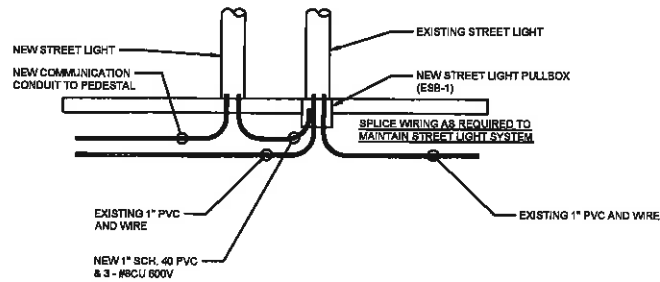
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RV80XSB8D
RIVERSIDE, CA 92508
(N) CONCRETE LIGHT POLE

SHEET TITLE
EQUIPMENT DETAILS

SHEET NUMBER
EQ-1



PROJECT NO: RV80XSB00
 DRAWN BY: MR
 CHECKED BY: JF

1	07/16/18	CLIENT COMMENTS
0	07/25/18	CONSTRUCTION DRAWINGS

IT IS A VIOLATION OF THE LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT

RV80XSB000
 RIVERSIDE, CA 92508
 (N) CONCRETE LIGHT POLE

SHEET TITLE
 EQUIPMENT DETAILS

SHEET NUMBER
 EQ-2

REPLACEMENT POLE DETAIL SCALE N.T.S. 1

ELECTRICAL SERVICE BOX SCALE N.T.S. 6

ELECTRICAL SERVICE BOX SCALE N.T.S. 9

ELECTRICAL SERVICE BOX SCALE N.T.S. 2

ELECTRICAL SERVICE BOX SCALE N.T.S. 4

ELECTRICAL SERVICE BOX SCALE N.T.S. 7

ELECTRICAL SERVICE BOX SCALE N.T.S. 10

ELECTRICAL SERVICE BOX SCALE N.T.S. 3

ELECTRICAL SERVICE BOX SCALE N.T.S. 5

ELECTRICAL SERVICE BOX SCALE N.T.S. 8

ELECTRICAL SERVICE BOX SCALE N.T.S. 11

(2) 6' ALUMINUM ELLIPTICAL LUMINAIRE ARMS AT 0° AND 180°.

TYPICAL #28" X 3'-0" HIGH MAXIMUM ANTENNA AND SHROUD ASSEMBLY (BY OTHERS).

LUMINAIRE NOM 30" ABOVE GRADE (BY OTHERS)

SIDE MOUNT RADIO TRANSMITTER AND/OR RRU FOR ANTENNA (BY OTHERS) AT 90° AND 270°.

(MODSX300) @ 0°
(MODSX300) @ 180°

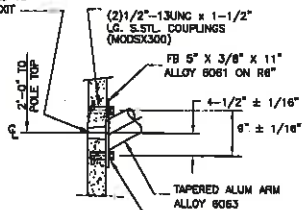


(2) 1/2"-13 x 3" STAR INS. 1 1/2" O.C. WITH 3" x 7" HANDHOLE W/ CAST ALUM. FRAME & FLUSH COVER FOR WIRE EXIT @ 90° AND 270°. (MOD A)

SIDE VIEW

1-1/2" DIA. TAPERED TO 1-1/4" DIA. WIRE EXIT

SPUN, PRESTRESSED CONCRETE POLE



BRACKET ARM TO POLE CONNECTION @ 0° AND 180°.

(3) INSERTS FOR #6 DRIVE SCREWS @ 0° (MOD24)

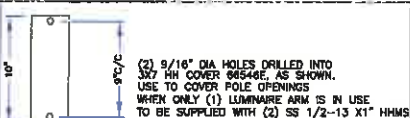
1-1/4" x 7-1/2"

3" x 7" HANDHOLE W/ FLUSH COVER @ 0° COVER TO BE ATTACHED BY AMERON PRIOR TO SHIPMENT WITH ONE-WAY (SCE) DOOR SCREWS

GROUND LINE

(4) 1-1/4" x 48" x 8" ANCHOR BOLTS, EACH W/ (2) HEX NUTS & (2) WASHERS; ASTM F1554 GR 55, GALV. TO ASTM A-153 ON 16" BC.

4.5 FT SQUARE BY 5 FOOT DEEP FOUNDATION (TYP) - SEE PROJECT SPECS FOR REQUIRED BASE SIZE ALONG WITH CONDUIT SIZING AND ORIENTATIONS REQUIRED.

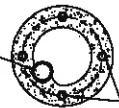


66546EM1

COMPLIANCE TO ANY CODE NOT EXPLICITLY LISTED ON THIS DRAWING IS NOT GUARANTEED. PLEASE CONTACT HOV AMERON IF YOU HAVE SPECIFIC CODE REQUIREMENTS.

(4) 3/4"-10UNC x 5" TOP MOUNT BOLTS WITH COUP. NUTS @ 90° ON 240mm (9.45") BOLT CIRCLE (MOD B) FOR TOP MOUNT ATTACHMENT WITH (4) 3/4"-10UNC SS HHMS & (4) 3/4" CUT WASHERS (BY OTHERS)

(1) 2" RIGID 26 FT. PVC CONDUIT CAST INTO POLE WALL (MOD-C)

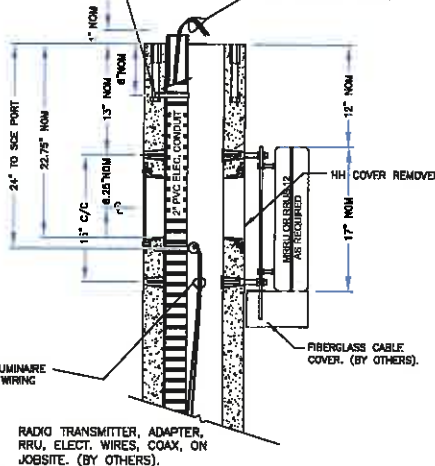


POLE TOP DETAIL

(4) 3/4"-10 COUP. NUTS WITH 5" BOLTS @ 90° ON 9.45" BC (MOD-B)

(18) 12" L. SS CABLE TIES 16" OC, FULL LENGTH

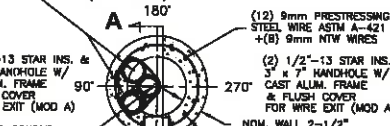
VERIZON 120V POWER (AS REQ'D)



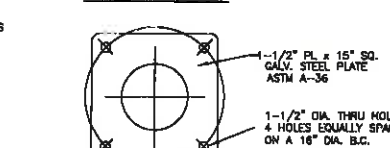
RADIO TRANSMITTER, ADAPTER, RRU, ELECT. WIRES, COAX, ON JOBSITE. (BY OTHERS).

POLE TOP CUT-AWAY DETAIL

CONDUIT SUPPORT ASSEMBLY (MODSX300) FOR LMA 180°



HH SECTION AND POLE ORIENTATION



BASE PLATE

SECTION A-A

REV.	DATE	DESCRIPTION	DRN.	APPR.
A	2/06/15	ADD DETAILS; LOWER INSERTS FOR XFMR, CHANGE XFMR ADAPTER DETAIL	SUB	
B	2/13/15	CHG. DETAILS; RAISE INSERTS FOR XFMR,	SUB	
C	2/17/15	ADD (8) NTW; INCREASE POLE CAPACITY	SUB	

"7" LEVEL CONFIG CODES			"P" LEVEL CONFIG CODES		
OPTION CLASS	ENTRY	INFO	OPTION CLASS	ENTRY	INFO
COATING	S		MIX	B1	
HH COVER	66546E	(3 REQ'D)	FINISH	5	
HH COVER	66546EM1	(1 REQ'D)	POLE TOP CON.	(4) MOD-B	3/4"-10 COUPNUTS
HH PAINT	BARE		COLLAR SHAPE	NONE	
DOOR SCR	SCE	(2 REQ'D)	BASEPLATE	63250E	
DOOR SCR	1/4-20X.75	(4 REQ'D)	CAST IN MOD	(2) MODSX300	SIDE MT.
DOOR SCR	1/2-13X.75	(2 REQ'D)	CAST IN MOD	MOD-A	PH + 1/2" STAR INS.
ORILL IN	MOD24		CAST IN MOD	(1) FOUNDATION MOD-C	TOP 26 FT
			CAST IN MOD	(1) SUPPORT	UP 30"

POLE DESIGNATION	POLE HEIGHT ABOVE GROUND	OVER-ALL LENGTH	ANCHOR BOLT CIRCLE	POLE DIA.	ULTIMATE G.L. MOMENT (ft. lbs.)	WEIGHT (lbs.)
BP300X09	29'-3"	29'-6"	16"	11-13/16"	79,900	2900

() POLES REQ'D (P/N: BP300X09-815S) INCL CONDUIT, EA WITH (2) 6' WALL BRACKET ALUM ELLIP. ARMS (P/N: LAEB6A); (1) ALUM COVER PLATE (66546EM1) FOR USE WHEN ONLY (1) LAEB6A ARM IS IN USE.

- NOTES:**
- MIX (815S): SCE BLACK & WHITE, LIGHTLY EXPOSED AGGREGATE FINISH, WITH FLAT, WATER SEALER COATING.
 - ASTM C-150 TYPE III GRAY CEMENT.
 - Fc @ 28 DAYS = 7,000 PSI, USING SPUN CYLINDER TEST.
 - Fc @ 28 DAYS = 5,000 PSI, USING ASTM C-31 CYLINDER TEST.
 - POLES MANUFACTURED PER ASTM C-1089-13 SPECIFICATIONS.
 - PROTECTIVE COAT EXPOSED P.C. WIRES AT POLE ENDS.
 - SCE MAX ANTENNA: 200 POUNDS; CENTERED 2 FT ABOVE POLE TOP. - MAXIMUM PROJECTED AREA - ROUND SHAPE IS 11 SQUARE FEET.
 - DUE TO THE NATURE & CHARACTERISTICS OF CONCRETE, SIDE MOUNT SPACING DIMENSIONS CAN ONLY BE TAKEN TO THE NEAREST 1/8 INCH.
 - INTERNAL SEPARATE ELECTRICAL CONDUIT FULL POLE LENGTH TO BE FURNISHED AND INSTALLED BY AMERON. INTERNAL ELEC. WIRES FOR ANTENNA AND RADIOS TO BE SEPERATED FROM SCE LUMINAIRE WIRES.

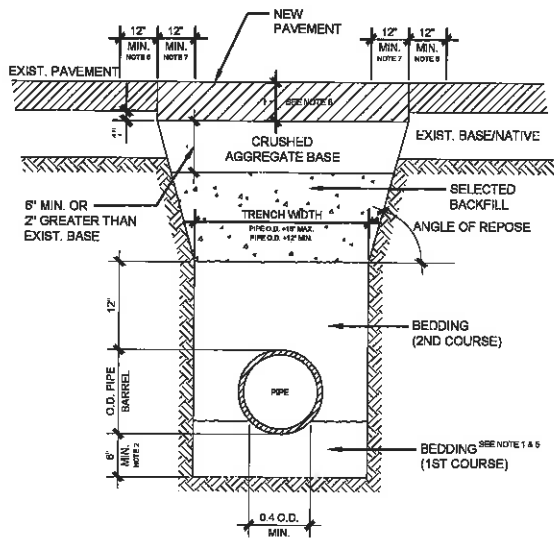
APPROVED BY _____ DATE _____

Almeron POLE PRODUCTS DIVISION

SOUTHERN CALIFORNIA EDISON/VERIZON
BP300X09 POLE WITH DOUBLE 6' ARMS
SPCL COVERED WIREWAYS FOR RADIOS

THIS DOCUMENT CONTAINS INFORMATION WHICH IS PROPRIETARY TO AMERON. IT SHALL NOT BE REPRODUCED, USED OR DISCLOSED TO ANYONE WITHOUT THE PRIOR WRITTEN PERMISSION OF AMERON.

DRAWN: SUB	2/15	SCALE	DWG. NO.	REV
CHK'D:		N.T.S.	1409-040	C



NOTES:

1. BEDDING MATERIAL SHALL FIRST BE PLACED SO THAT THE PIPE IS SUPPORTED FOR FULL LENGTH OF THE BARREL WITH FULL BEARING (0.4 O.D. MIN.).
2. IN CASES WHERE NATIVE FREE-DRAINING GRANULAR MATERIAL IS SUITABLE FOR USE AS BEDDING, THE TRENCH MAY BE EXCAVATED TO A POINT ABOVE THE INVERT GRADE AND THE TRENCH BOTTOM HAND-SHAPED SO THAT THE BOTTOM SEGMENT OF THE PIPE IS FIRMLY SUPPORTED ON UNDISTURBED MATERIAL.
3. IF THE MAXIMUM TRENCH WIDTH IS EXCEEDED, ADDITIONAL BEDDING, ANOTHER TYPE BEDDING OR A HIGHER STRENGTH OF PIPE SHALL BE PROVIDED AS DETERMINED BY THE ENGINEER.
4. BEDDING AND BACKFILL MATERIAL SHALL BE COMPACTED TO A MINIMUM OF 90% RELATIVE COMPACTION, WITH THE EXCEPTION OF THE UPPER SIX (6) INCHES OF SUBGRADE WHICH SHALL BE COMPACTED TO A MINIMUM OF 85% RELATIVE COMPACTION. COMPACTION METHODS SHALL COMPLY WITH STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION.
5. BEDDING MATERIAL SHALL BE SAND.
6. PRIOR TO INSTALLING FINISH SURFACE COURSE, ASPHALT BASE COURSE PLUS ADDITIONAL MINIMUM 12" WIDTH OF EXISTING PAVEMENT ON BOTH SIDES OF MAIN AND LATERAL EXCAVATIONS SHALL BE COLD PLANED 90 AS TO RESULT IN A MINIMUM FINISH SURFACE THICKNESS OF 1.5" AND RECEIVE AN APPLICATION OF TACK COAT.
7. SAW CUT AND REMOVE 12" TO CREATE A STRAIGHT EDGE PRIOR TO INSTALLING A.C. BASE COURSE.
8. T = PAVEMENT THICKNESS = EXIST. +1" WITH A 4" MIN. FOR A.C. OR 6" MIN. FOR PCC.
BASE COURSE = C2-AR-4000
MIN 1.5" THICK FINISH COURSE = O2-AR-4000

TRENCH SECTION DETAIL

SCALE: NOT TO SCALE

1

NOT USED

SCALE: NOT TO SCALE

2



PROJECT NO: RV80XS080D
DRAWN BY: MR
CHECKED BY: JF

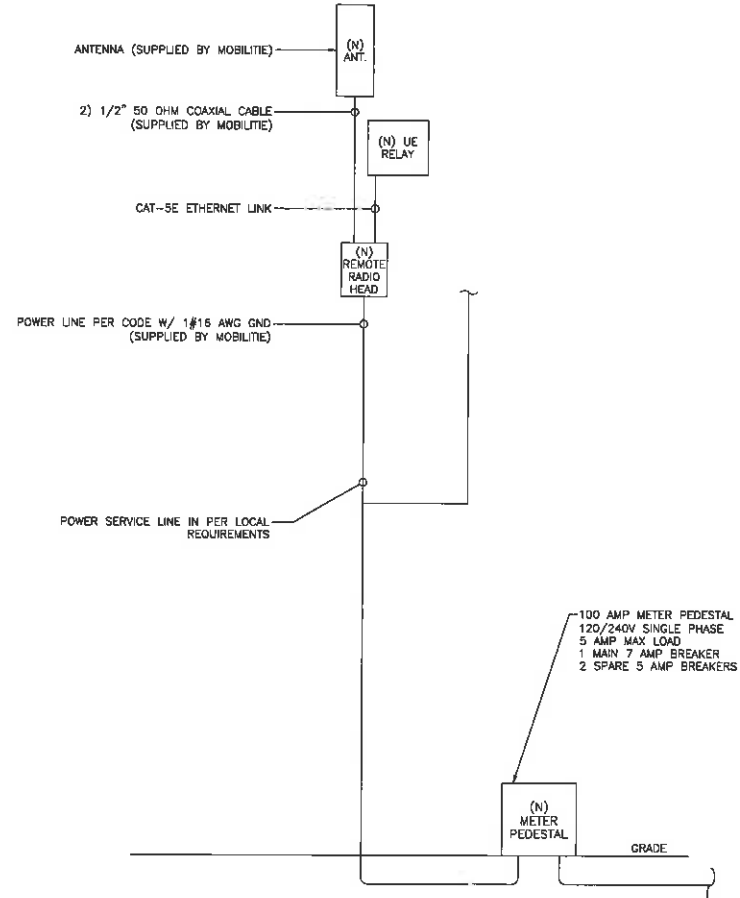
1 07/18/16 CLIENT COMMENTS
2 07/25/16 CONSTRUCTION DRAWINGS

IT IS A VIOLATION OF THE LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT

RV80XS080D
RIVERSIDE, CA 92508
(N) CONCRETE LIGHT POLE

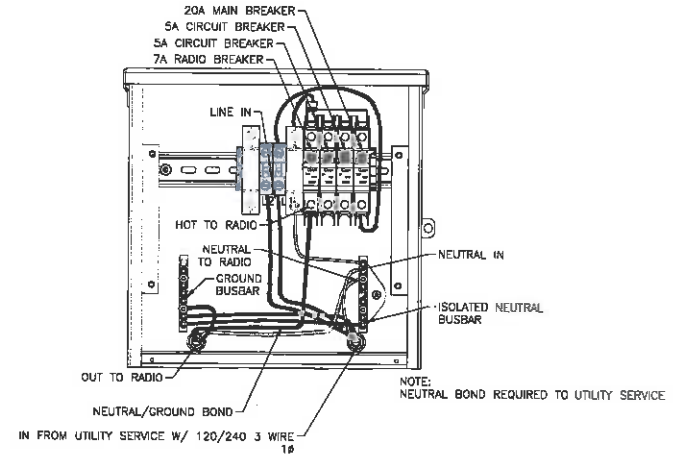
SHEET TITLE
EXCAVATION DETAILS

SHEET NUMBER
EX-1



ONE-LINE DIAGRAM
SCALE: NOT TO SCALE

1



BREAKER SCHEDULE
SCALE: NOT TO SCALE

2

VOLTAGE: 120V
PHASE & WIRE: 1PH / 2W
MAIN: 60A
BUS: 60A

LOCATION: MOUNTED ON (N) CONCRETE LIGHT POLE
NEMA: NEMA-4X
CKT CODE: 2 - EQUIPMENT
AIC RATING: 10,000

VOLT AMPS		DESCRIPTION	POLE	BKR	CKT	A		B		CKT	BKR	POLE	DESCRIPTION	VOLT AMPS	
PHASE A	PHASE B					PHASE B	PHASE A	PHASE B	PHASE A						
250	-	NOKIA B41 HP	1	7	1	●				2	5	1	SPARE	-	-
-	-	SPARE	1	5	3		●			4	-	-	-	-	-
-	-	-	-	-	5			●		6	-	-	-	-	-
250	-	VA/LINE										-	-		
PHASE A = 250						PHASE B = 0									

CONNECTED LOAD = 250 VA
DESIGN AMP. = 1.5A

NOTE:
RECTIFIER (AC/DC CONVERTER) IS FURNISHED AND INSTALLED INSIDE REMOTE RADIO HEAD (RRH)

PANEL SCHEDULE
SCALE: NOT TO SCALE

3



PROJECT NO: RV90XB06D
DRAWN BY: MR
CHECKED BY: JF

1	07/15/18	CLIENT COMMENTS
2	07/25/18	CONSTRUCTION DRAWING

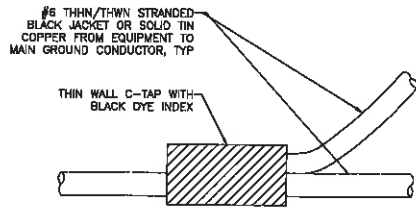
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RV90XB06D
RIVERSIDE, CA 92608
(N) CONCRETE LIGHT POLE

SHEET TITLE
ELECTRICAL

SHEET NUMBER
E-1

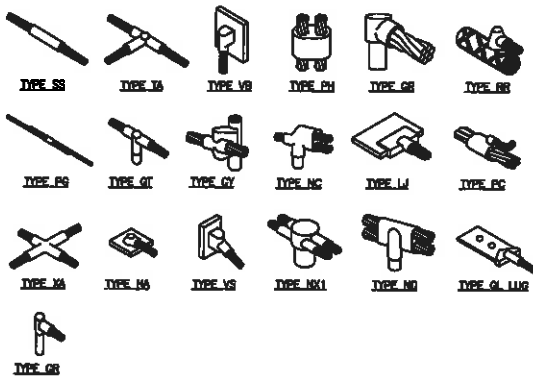
NOTE:
CONTRACTOR TO SURROUND COMPLETED CONNECTION WITH HEAT-SHRINK TUBING TO ENSURE WEATHER PROOF CONNECTION



C-TAP DETAIL

SCALE: NOT TO SCALE

1

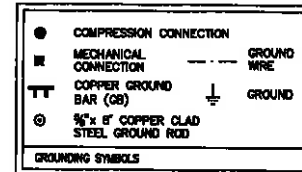
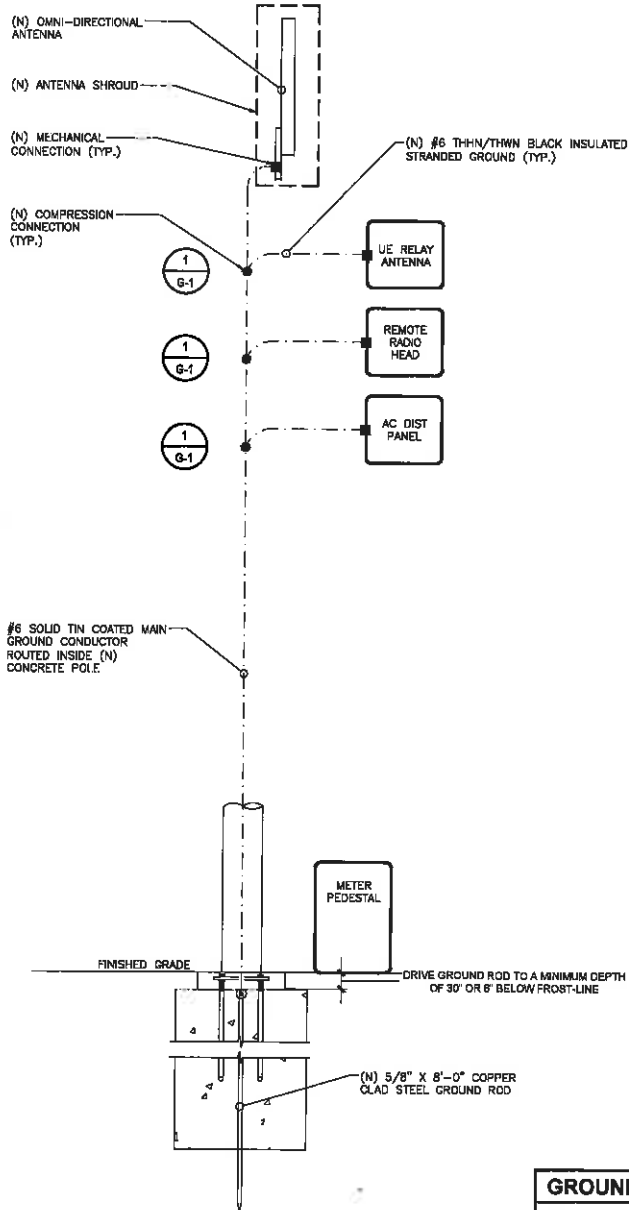


- GROUNDING BONDS:** ALL BONDS ARE TO BE MADE WITH #12 AWG STRANDED COPPER IN BLACK INSULATION. (ATT-TP-78416 7, 6.7)
- EXTERIOR UNIT BONDS:** ALL METALLIC OBJECTS SHALL BE BONDED TO THE GROUND ROD. (ATT-TP-78416 7, 12.8)
- GROUND ROD:** UL LISTED COPPER CLAD STEEL GROUND ROD WITH MINIMUM DIAMETER OF 5/8" AND MINIMUM LENGTH OF 8 FEET. ALL GROUND RODS MAY BE INSTALLED WITH INSPECTION SLEEVES. GROUND RODS SHALL BE DRIVEN TO A MINIMUM DEPTH OF 30" BELOW GRADE OR 6 INCHES BELOW FROST LINE. (ATT-TP-78416 1.4 / 2.2, 3, 10)

WELD CONNECTION DETAILS

SCALE: NOT TO SCALE

2



NOTES:

- GROUNDING RISER FOR DIAGRAMMATIC PURPOSES ONLY. SEE ELEVATION DRAWING FOR EQUIPMENT AND ANTENNA LOCATIONS.
- ALL RGS TO BE GROUNDED AT BOTH ENDS USING GROUNDING BUSHINGS
- GROUND WIRE BELOW 11' AGL TO BE RUN IN 1/2" SCHEDULE 40 PVC

GROUNDING RISER DIAGRAM

SCALE: NOT TO SCALE

3

mobilite
Intelligent Infrastructure

XD industries
1872 N. BARTONVILLE DR. STE. 10
ORANGE, CA 92667

PROJECT NO: RV90XS808D
DRAWN BY: MR
CHECKED BY: JF

1 07/18/18 CLIENT COMMENTS
2 07/26/18 CONSTRUCTION DRAWINGS

IT IS A VIOLATION OF THE LAW FOR ANY PERSON UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT

RV90XS808D
RIVERSIDE, CA 92508
(N) CONCRETE LIGHT POLE

SHEET TITLE
GROUNDING DETAILS

SHEET NUMBER
G-1

**NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Thursday from 8:00 a.m. to 5:00 p.m., except Monday, October 8 (Columbus Day), and by prescheduled appointment on Fridays from 9:00 a.m. to 5:00 p.m. Please send any written comments to the above address.

ATTENTION: ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. The March Joint Powers Authority may hold hearings on this project and should be contacted on non-ALUC issues.

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside California

DATE OF HEARING: October 11, 2018

TIME OF HEARING: 9:30 A.M.

CASE DESCRIPTION:

ZAP1327MA18 – Mobilitie, LLC (Representative: Robert Schultz) – March Joint Powers Authority Case No. CUP18-01 (Conditional Use Permit). A proposal to remove an existing 26 foot tall street light pole and replace it with a new 33.3 foot tall wireless facility street light pole (and associated equipment) located on the westerly side of Innovation Drive, within the street right-of-way southerly of Cactus Avenue. (Airport Compatibility Zone B2 of the March Air Reserve Base/Inland Port Airport Influence Area).

FURTHER INFORMATION: Contact Paul Rull at (951) 955-6893. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Mr. Mathew Evans of the March Joint Powers Authority at (951) 656-7000.



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP 1327 MA 18 DATE SUBMITTED: August 16, 2018

March
B2

APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

Applicant	Mobilitie, LLC	Phone Number (909) 732-5615
Mailing Address	2955 Red Hill Ave., Suite 200 Costa Mesa, CA 92626	Email rschultz@mobilitie.com
Representative	Robert Schultz	Phone Number (909) 732-5615
Mailing Address	2955 Red Hill Ave., Suite 200 Costa Mesa, CA 92626	Email rschultz@mobilitie.com
Property Owner	N/A - Located in public right-of-way.	Phone Number N/A
Mailing Address		Email N/A

LOCAL JURISDICTION AGENCY

Local Agency Name	March JPA	Phone Number (951) 656-7000
Staff Contact	Dan Fairbanks, Planning Director <i>(Mathew Evans)</i>	Email fairbanks@marchjpa.com
Mailing Address	14205 Meridian Parkway, Suite 140 Riverside, CA 92518	Case Type
Local Agency Project No	<u>CUP 18-01</u>	<input type="checkbox"/> General Plan / Specific Plan Amendment <input type="checkbox"/> Zoning Ordinance Amendment <input type="checkbox"/> Subdivision Parcel Map / Tentative Tract <input type="checkbox"/> Use Permit <input type="checkbox"/> Site Plan Review/Plot Plan <input checked="" type="checkbox"/> Other

PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address	In the right-of-way on Innovation Dr., south of the intersection of Cactus Ave. and Innovation Dr. Located approximately at Latitude/Longitude: 33.90776/ -117.285062		
Assessor's Parcel No.	N/A	Gross Parcel Size	N/A
Subdivision Name	N/A	Nearest Airport	March Air Reserve Base, approximately 2.5 miles
Lot Number	N/A	and distance from Airport	

PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe)	Existing 29 ft., 3-inch concrete street light utility pole in the right-of-way. The existing street light is located approximately at Latitude/Longitude: 33.90776/ -117.285062.

Proposed Land Use (describe)	Mobilite, LLC's submission is for a 4 ft. Shrouded Small Cell Mini-Macro (MM) wireless antenna attached to a new replacement pole consisting of a 29 ft., 3-inch concrete "dual use" wireless & street light utility pole for an overall structure height of 33 feet, 3 inches, located in the public right-of-way of Innovation Dr. The existing concrete street light utility pole is located approximately at Latitude/Longitude: 33.90776/ -117.285062, known as Site ID/Candidate Code: 9CAB012636D/RV90XSB08D.		
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)	N/A	
For Other Land Uses (See Appendix C)	Hours of Operation	N/A	
	Number of People on Site	N/A	Maximum Number N/A
	Method of Calculation		
Height Data	Site Elevation (above mean sea level)	1568	ft.
	Height of buildings or structures (from the ground)	33.2	ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No
	If yes, describe	_____	

- A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:**
- 1. Completed ALUC Application Form
 - 1. ALUC fee payment
 - 1. Plans Package (24x36 folded) (site plans, floor plans, building elevations, landscaping plans, grading plans, subdivision maps)
 - 1. Plans Package (8.5x11) (site plans, floor plans, building elevations, landscaping plans, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
 - 1. CD with digital files of the plans (.pdf) *stick*
 - 1. Vicinity Map (8.5x11)
 - 1. Detailed project description
 - 1. Local jurisdiction project transmittal
 - 3. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
 - 3. Gummed address labels of all surrounding property owners within a 300 foot radius of the project site (**only required if the project is scheduled for a public hearing Commission meeting**). If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10) with ALUC return address. *

* Projects involving heliports/helicopter landing sites will require additional noticing procedures.



2955 Red Hill Avenue
Suite 200
Costa Mesa, CA 92626
Tel. 877.296.1009
www.mobilitie.com

August 14, 2018

Riverside County ALUC
County Administrative Center
4080 Lemon Street, 14th Floor
Riverside, CA 92501

RE: APPLICATION FOR MAJOR LAND USE ACTION REVIEW REQUEST FOR MOBILITIE, LLC'S WIRELESS UTILITY POLE IN THE PUBLIC RIGHT-OF-WAY OF INNOVATION DRIVE.

To Whom it May Concern,

Thank you for your time to discuss Mobilitie, LLC's wireless utility plans within the March Joint Powers Authority (JPA) jurisdiction located in Riverside County near the March Air Reserve Base. As discussed, Mobilitie is a State of California certificated Utility Company that develops intelligent infrastructure solutions to densify and optimize wireless carriers' services and geographic reach.

Specifically, Mobilitie is deploying Mini-Marco (MM) Small Cell Wireless Utility Poles within the public right-of-way throughout California and across the United States. This infrastructure works together to form a vast network that will provide high-speed, high-capacity bandwidth and mobile device connectivity to serve the residents, businesses and visitors of Riverside County and March JPA.

SMALL CELLS SITES

Mobilitie's Small Cell Sites involves the installation of small, low-powered wireless facilities, consisting of a transmit-receive antenna that communicates with wireless devices, a wireless backhaul antenna that connects the facility to the carrier's core network, and compact radio equipment mounted on either new or existing utility or light poles. These Small Cell sites add coverage, density and capacity to the existing wireless networks, and are designed to blend with existing infrastructure.

REQUEST FOR APPROVAL

Mobilitie, LLC's respectfully requests that the Riverside County Airport Land Use Commission review and approve the enclosed application. This submission is for a 4 ft. Small Cell Mini-Macro (MM) Wireless Antenna attached to a new street light replacement pole being a 29 ft., 3-inch concrete street light utility pole for an overall structure height of 33 feet, 3 inches, located in the Public Right-of-Way (PROW) of Innovation Dr., known as Site ID/ Candidate Code: 9CAB012636D/RV90XSB08D.

Mobilitie values the ALUC's input on our proposal, and will work closely with your staff to determine the optimal locations that balances the County's principles that guide the use and management of its public rights-of-way in a reasonable and proficient manner.

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.6

HEARING DATE: October 11, 2018

CASE NUMBER: ZAPI328MA18 – Truck Terminal Properties (Representative: Joseph E. Bonadiman & Associates, Inc.)

APPROVING JURISDICTION: March Joint Powers Authority

JURISDICTION CASE NOS: Change of Zone 17-01, Plot Plan 17-07

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends that the Commission find the proposed Change of Zone CONSISTENT with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, and find the proposed Plot Plan CONDITIONALLY CONSISTENT, subject to the conditions included herein, and such additional conditions as may be required by the Federal Aviation Administration Obstruction Evaluation Service.

PROJECT DESCRIPTION: The applicant proposes to establish a truck parking terminal consisting of 258 truck trailer parking spaces (and 3 regular vehicle parking spaces) totaling 11.2 acres of paved parking area, a 450 square foot security office, and 0.36 acres of basin area, on a 13.68 acre site. The applicant also proposes a change of zone for the site to Industrial.

PROJECT LOCATION: The site is located westerly of Heacock Street, northerly of the westerly extension of San Michele Road, and easterly of the March Air Reserve Base, within the jurisdiction of the March Joint Powers Authority, approximately 2,100 feet easterly of Runway 14-32 at March Air Reserve Base.

LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

- a. Airport Influence Area: March Air Reserve Base
- b. Land Use Policy: Zone B2
- c. Noise Levels: 60 – 75 range CNEL from aircraft

BACKGROUND:

Non-Residential Average Land Use Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zone B2, which limits average intensity to 100 people per acre.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, the following rate was used to calculate the occupancy for the proposed buildings in Compatibility Zone B2:

- Office – 1 person per 200 square feet (with 50% reduction)

The project proposes a 450 square foot security office, which will accommodate an occupancy of 2 people, which would result in an average intensity of less than 1 person per acre, which is consistent with Compatibility Zone B2 criterion of 100.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle and 1.0 persons per truck trailer parking in the absence of more precise data). Based on the 3 parking stalls and 258 truck trailer stalls provided, the total occupancy would be estimated to be 263 people. The resulting average intensity of 19 people per acre is consistent with the Compatibility Zone B2 average criterion of 100.

Non-Residential Single-Acre Land Use Intensity: Compatibility Zone B2 limits maximum single-acre intensity to 250 people. There are no risk-reduction design bonuses available, as March Air Reserve Base/Inland Port Airport is primarily utilized by large aircraft weighing more than 12,500 pounds.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre area would be located around the 450 square foot security office building, accommodating 2 people, which is consistent with the Compatibility Zone B2 single acre criterion of 250.

March Air Reserve Base/United States Air Force Input: Given that the project site is located in Zone B2 easterly of the runway at March Air Reserve Base, the March Air Reserve Base staff was notified of the project and sent a package of plans for their review. As of the time this staff report was prepared, we were still awaiting comments from the Air Force regarding this project.

The 2018 Airport Installations Compatible Use Zones (AICUZ) study identifies the Base's secondary runway (Runway 12-30) and its Clear Zones (CZ) and Accident Potential Zones (APZs). A small portion of the project's most northeasterly corner, which includes proposed parking spaces, drive aisles, landscaping, perimeter walls and fencing, appears to be located within APZ I. Appendix A of the AICUZ provides Land Use Compatibility Tables for the CZs and APZs, which cite "automobile parking" as a permitted use in APZ I and II (and prohibited use in the CZ). (Note: Runway 12-30 was not a factor in the development of Compatibility Zones adopted through the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan [ALUCP].) The 2014 March Air Reserve

Base/Inland Port ALUCP will need to be amended so as to be consistent with the 2018 AICUZ. However, until such time, the 2014 March ALUCP is the current adopted plan.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zone B2.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being in an area within the 65-75 CNEL range from aircraft noise. As a primarily industrial vehicle parking lot not sensitive to noise, special measures to mitigate aircraft-generated noise would not be needed. However, the proposed security office building would require special measures to mitigate aircraft-generated noise to an internal level of 45 CNEL or below, and, therefore, a condition for adequate noise attenuation within the office areas of the building is included.

Part 77: The elevation of Runway 14-32 at its southerly terminus is 1,488 feet above mean sea level (1,488 feet AMSL). At a distance of approximately 2,100 feet from the runway to the closest point within the site, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with top of roof exceeding 1,509 feet AMSL. The site is approximately 1,490 feet AMSL. With a maximum structure height (light pole) of 25 feet, the top point elevation would be 1,515 feet AMSL. Therefore, review of any structures exceeding 19 feet is required. Submittal to the FAAOES was made and Aeronautical Study Number 2018-AWP-14284-OE was assigned to this project. At the time of writing of this staff report, no determination has been made, but the study is in a "Work in Progress" status.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

Basins: There is an existing basin located on the southerly end of the parcel and the applicant proposes an additional basin for water quality management adjacent thereto (0.36 acres). Due to the proximity of these basins to the runway, there could be a potential for bird strikes. Therefore, recommended conditions require the new basin to be designed so as to provide for a maximum 48-hour detention period following the conclusion of a storm event, and to remain totally dry between rainfalls.

CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an

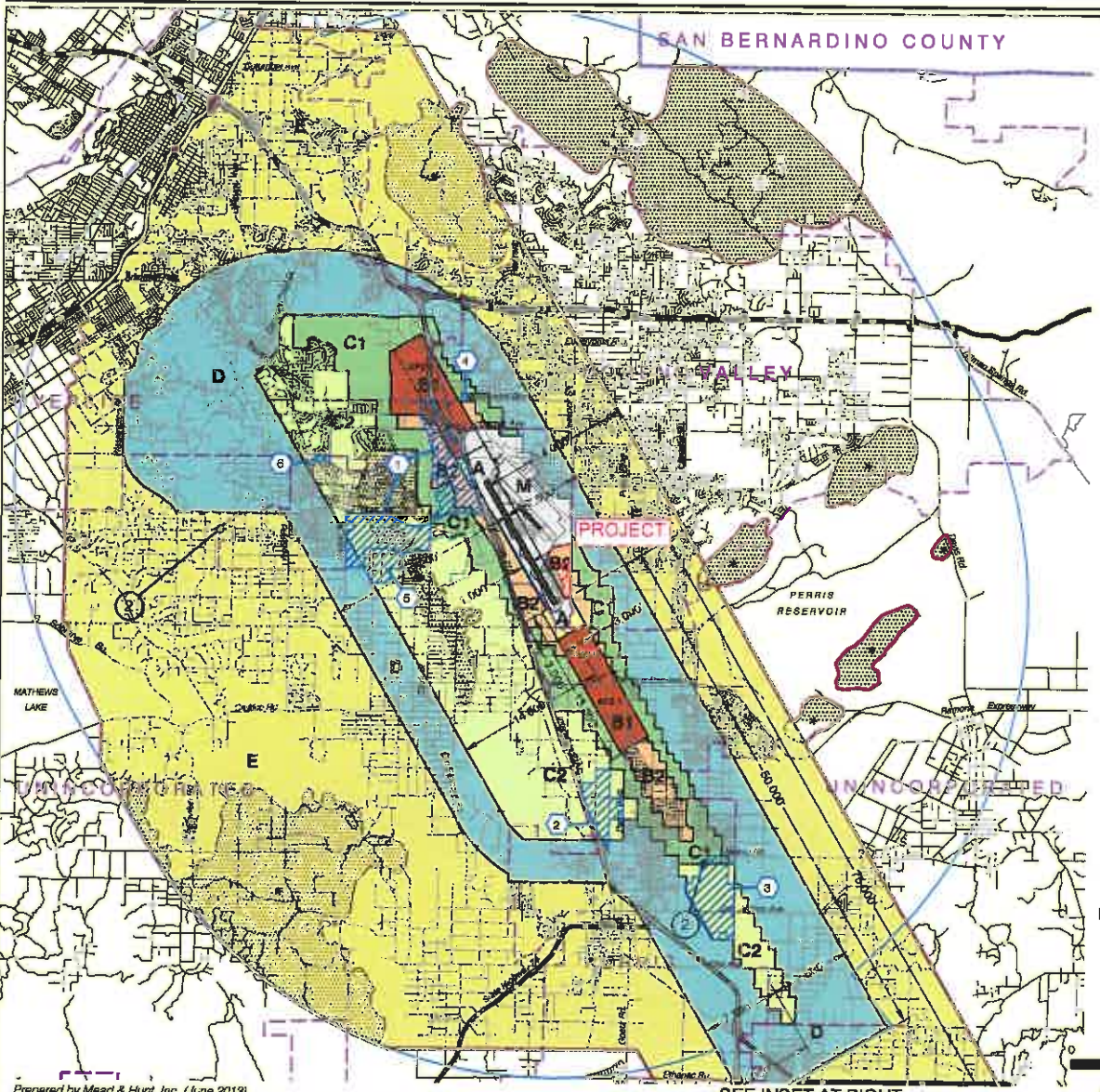
initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.

- (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) The storage, processing, or manufacturing of hazardous materials.
 - (f) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, congregate care facilities, hotels/motels, restaurants, places of assembly (including churches and theaters), buildings with more than 3 aboveground habitable floors, noise sensitive outdoor nonresidential uses, critical community infrastructure facilities and hazards to flight.
3. Prior to issuance of any building permits, the landowner shall convey and have recorded an aviation easement to the March Inland Port Airport Authority. Contact March Joint Powers Authority at (951) 656-7000 for additional information.
 4. The attached notice shall be provided to all prospective purchasers of the property and tenants or lessees of the building.
 5. The proposed water quality management basin/detention basin on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping. Trees shall be spaced to avoid creation of a continuous canopy. Landscaping in and around the detention basin shall not include vegetation that produces seeds, fruits, or berries.

6. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
7. Noise attenuation measures shall be incorporated into the design of the security office building, to the extent such measures are necessary to ensure that interior noise levels from aircraft operations are at or below 45 CNEL.
8. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission and March Air Reserve Base.
9. The lease between the March Joint Powers Authority and the applicant (or any future tenants) shall include a provision stating that the U.S. Air Force has the right and authority to inspect the premises and any vehicles located thereon without prior notice, as needed for security of its operations and personnel in its sole discretion.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



LEGEND

Compatibility Zones

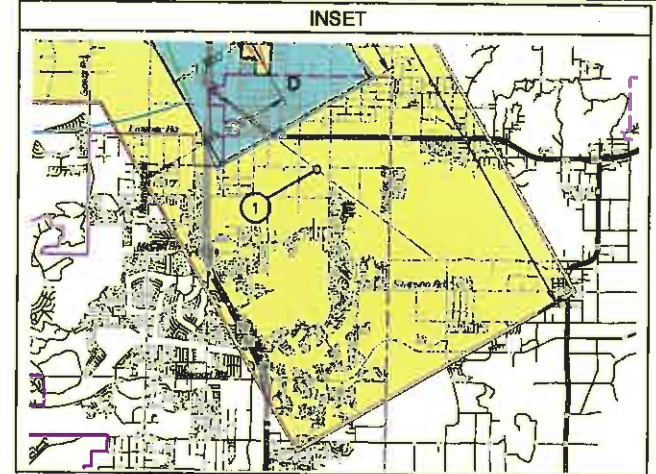
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,536 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



Riverside County
Airport Land Use Commission

March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)

Note:
All dimensions are measured from
runway ends and centerlines.



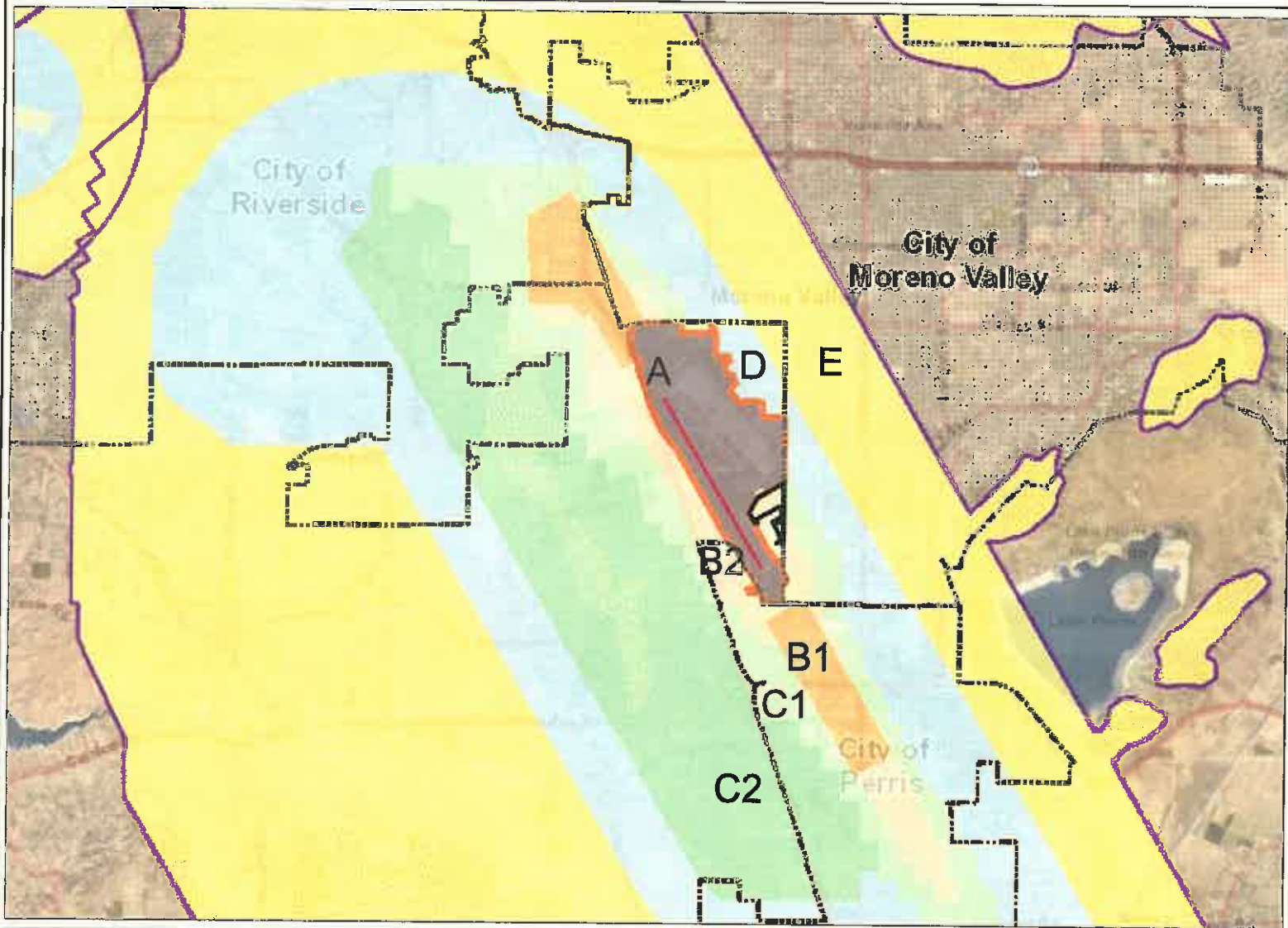
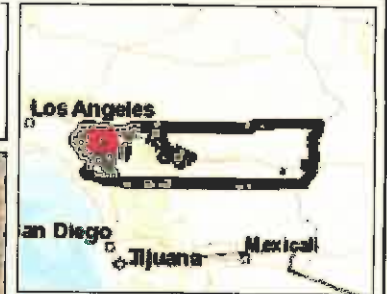
Base map source: County of Riverside 2013

Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map MA-1
Compatibility Map
March Air Reserve Base / Inland Port Airport

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-FXC5
- C2-EXC8



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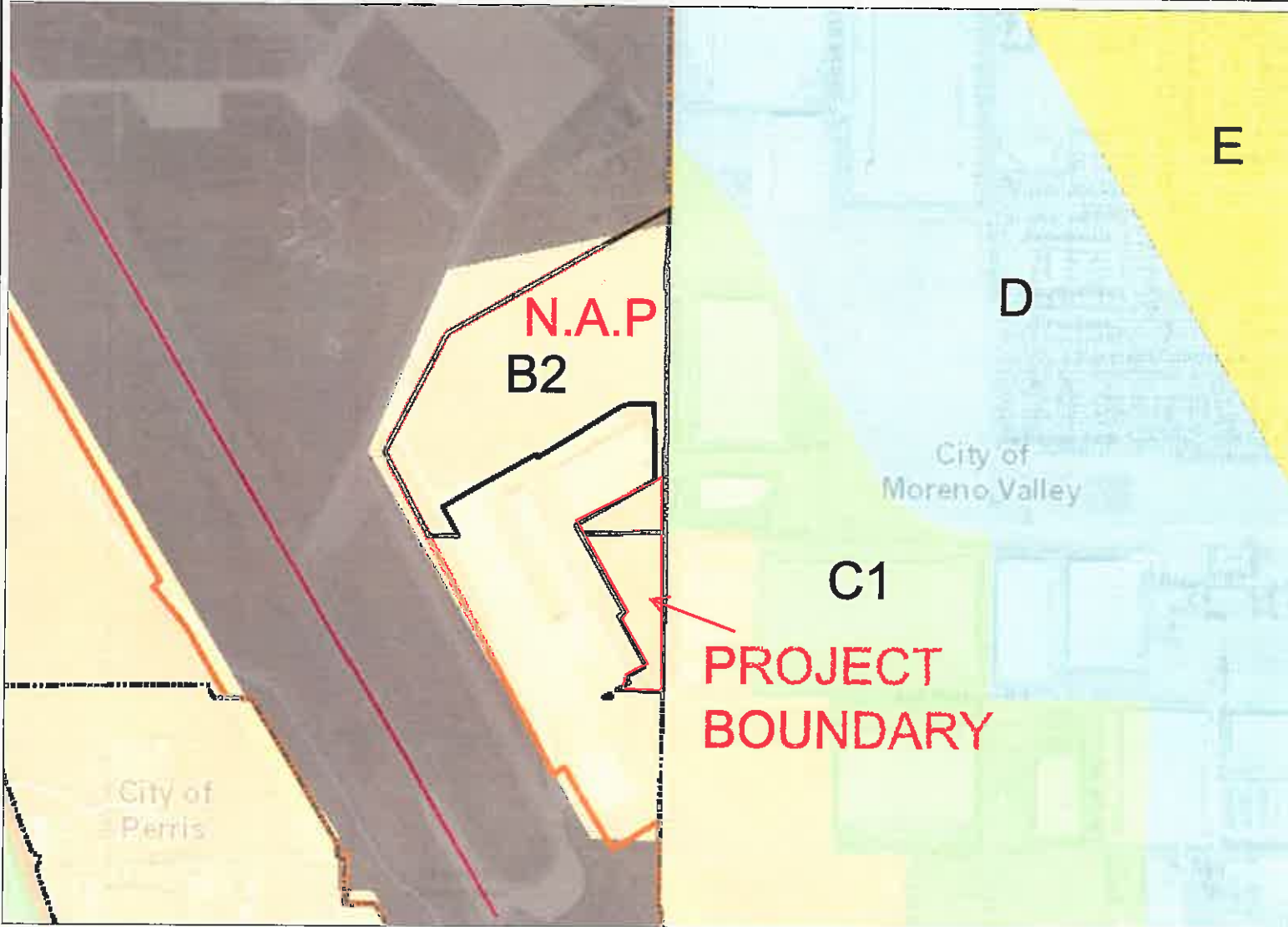
Notes



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Map My County Map



- Legend**
- Runways
 - Airports
 - Airport Influence Areas
 - Airport Compatibility Zones**
 - OTHER COMPATIBILITY ZONE
 - A
 - A-EXC1
 - B1
 - B1-APZ I
 - B1-APZ I-EXC1
 - B1-APZ II
 - B1-APZ II-EXC1
 - B1-EXC1
 - B2
 - B2-EXC1
 - C
 - C1
 - C1-EXC1
 - C1-EXC3
 - C1-EXC4
 - C1-HIGHT
 - C2
 - C2-EXC1
 - C2-EXC2
 - C2-EXC3
 - C2-EXC5
 - C2-EXC6



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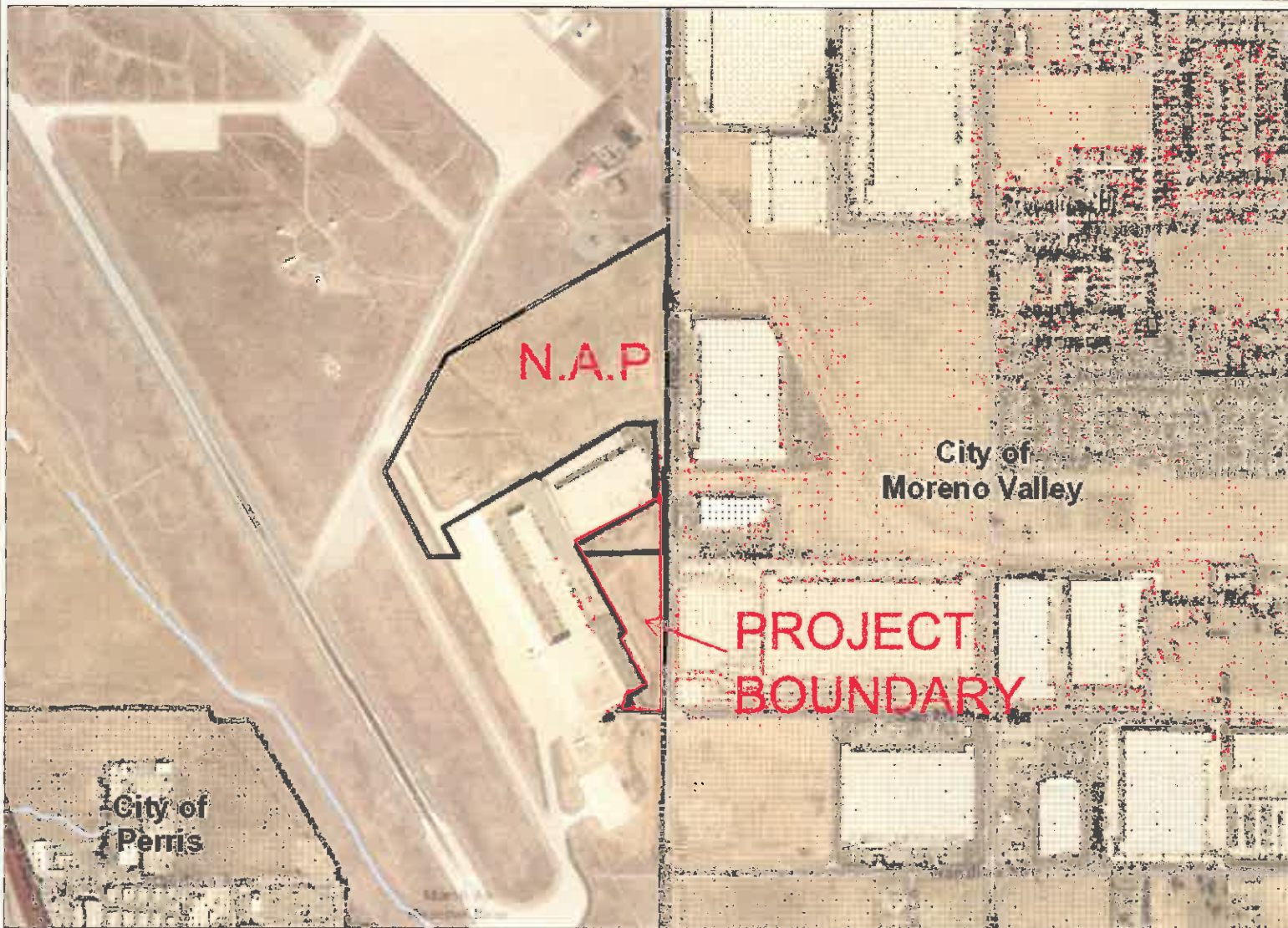


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Notes

Map My County Map



Legend

-  Blueline Streams
-  City Areas
-  World Street Map

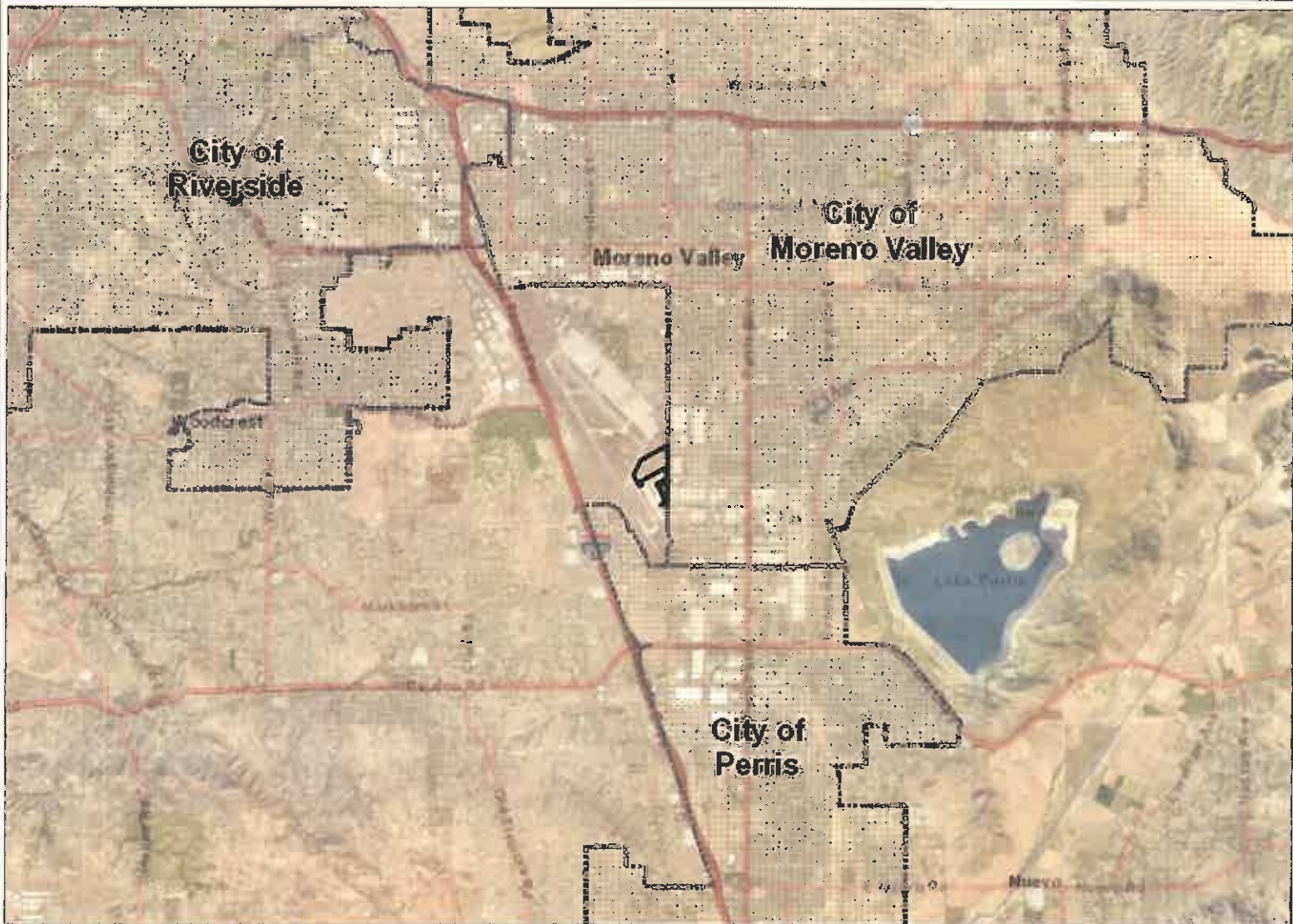
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Map My County Map



Legend

- City Areas
- World Street Map

Notes



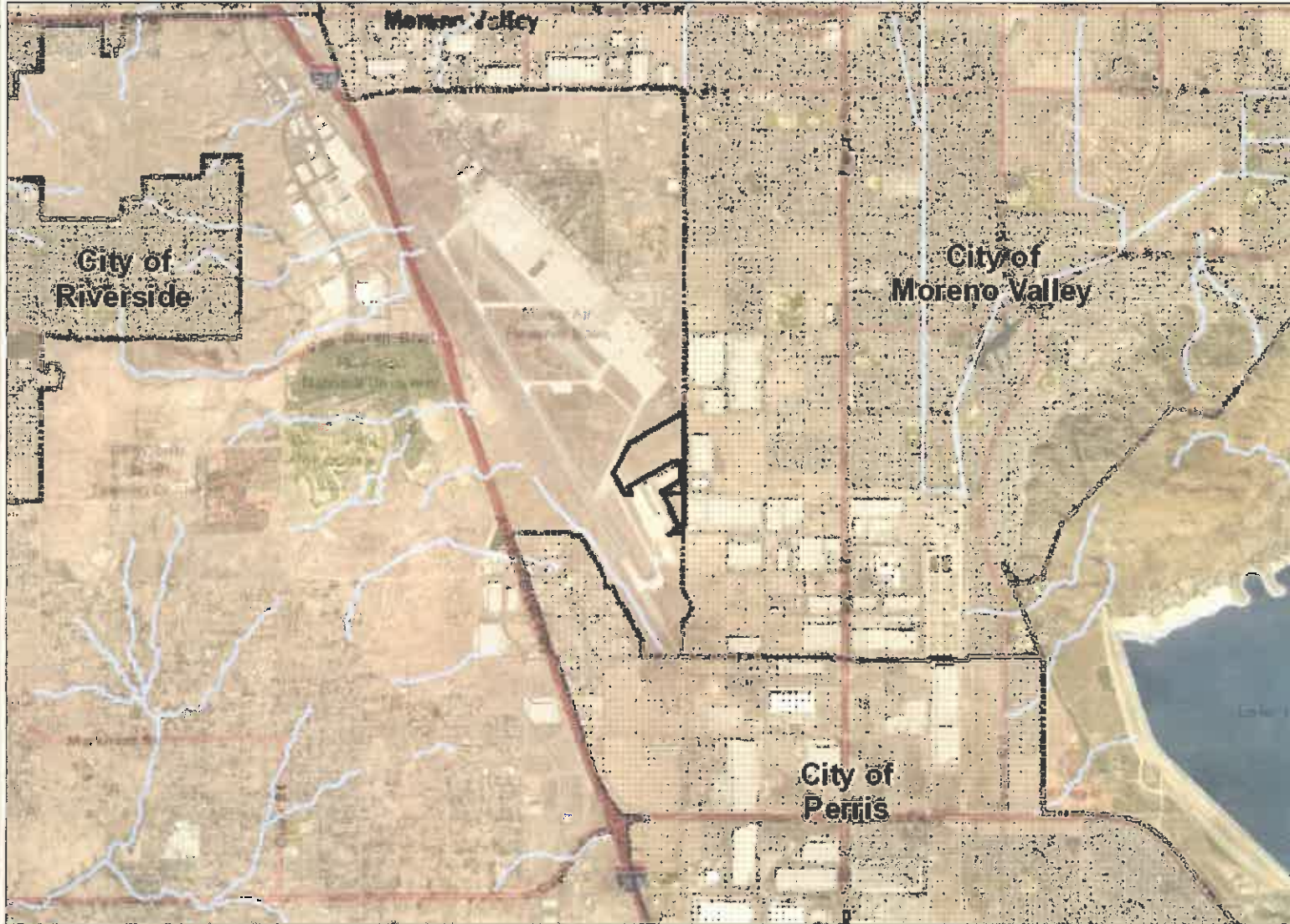
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Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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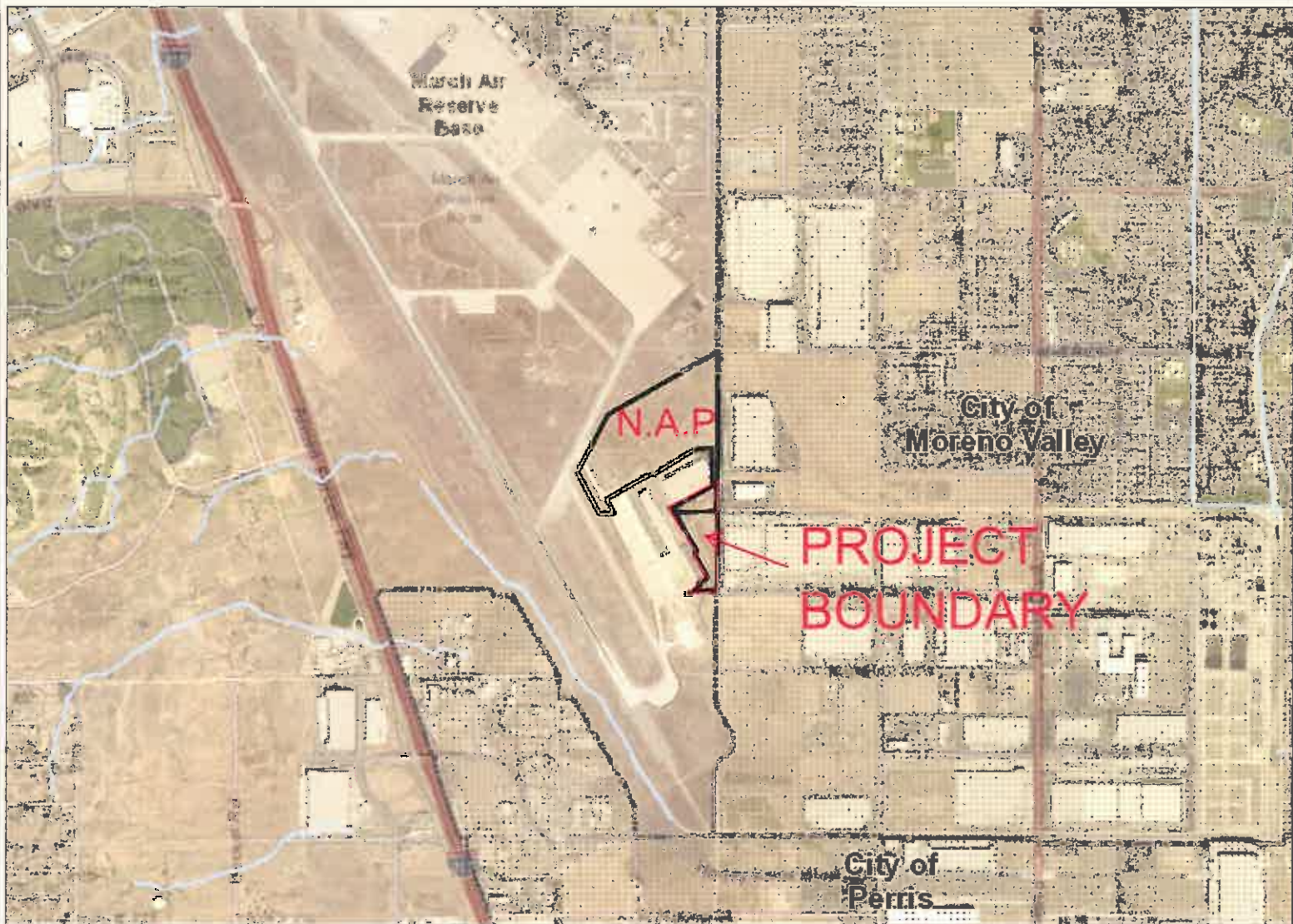
Notes



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Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map

Notes



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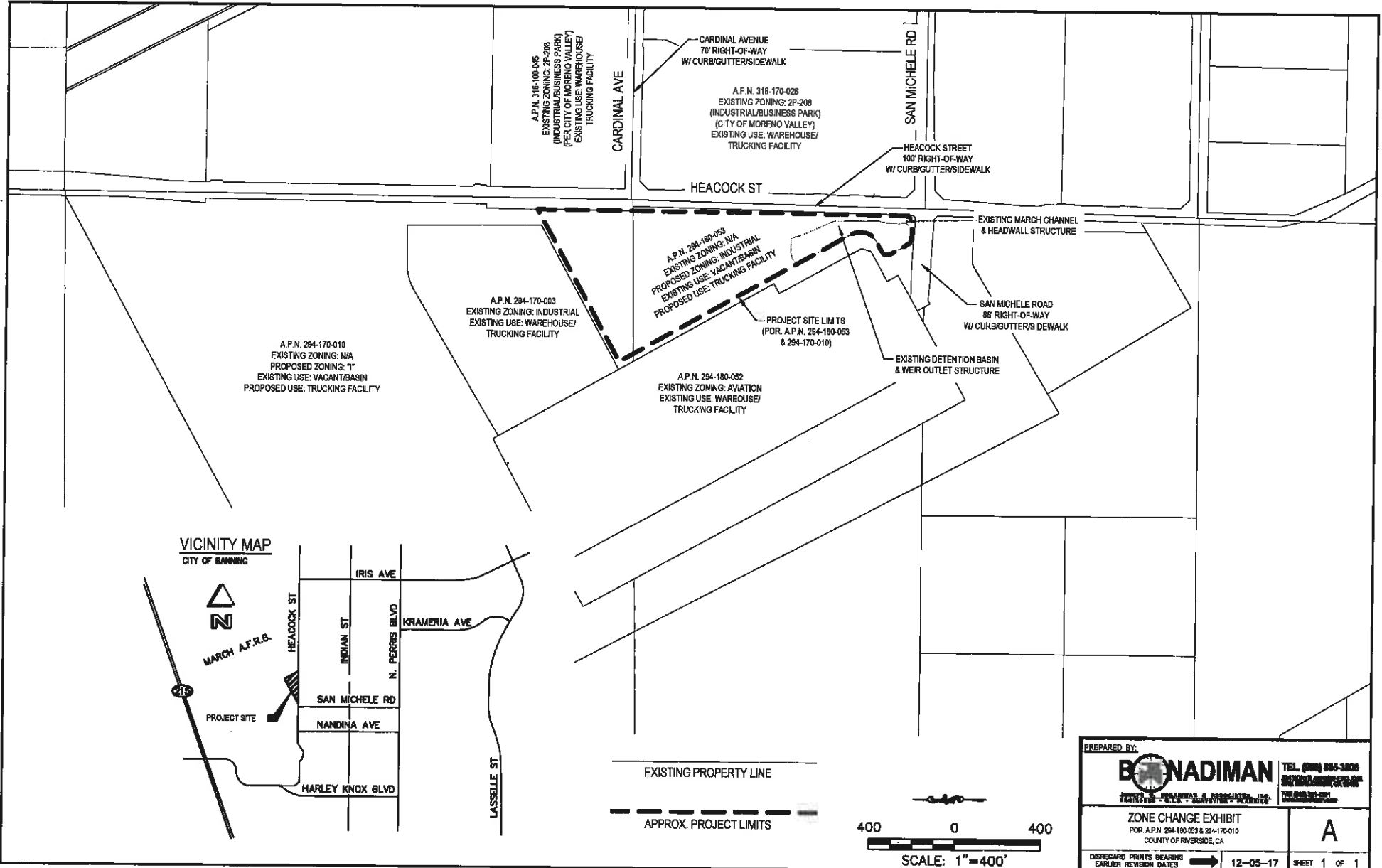
Legend

- Runway
- City of Moreno Valley
- City of Pems
- APZ I
- APZ II
- CZ



1 inch = 0.29 miles

SLUCM NO.	LAND USE NAME	CLEAR ZONE Recommendation ¹	APZ-I Recommendation ¹	APZ-II Recommendation ¹	DENSITY Recommendation ¹
46	Automobile parking	N	Y ⁶	Y	Maximum FAR of 0.28 in APZ I & 0.56 in APZ II
47	Communication	N	Y ⁶	Y	Maximum FAR of 0.28 in APZ I & 0.56 in APZ II
48	Utilities ⁷	N	Y ⁶	Y ⁶	Maximum FAR of 0.28 in APZ I & 0.56 in APZ II
48.5	Solid waste disposal (landfills, incinerators, etc.)	N	N	N	
49	Other transportation, communication, and utilities	N	Y ⁶	Y	See Note 6 below
50			Trade		
51	Wholesale trade	N	Y	Y	Maximum FAR of 0.28 in APZ I & 0.56 in APZ II
52	Retail trade – building materials, hardware and farm equipment	N	Y	Y	See Note 8 below
53	Retail trade – including, discount clubs, home improvement stores, electronics superstores, etc.	N	N	Y	Maximum FAR of 0.16 in APZ II
53	Shopping centers- Neighborhood, Community, Regional, Super-regional ⁹	N	N	N	
54	Retail trade – food	N	N	Y	Maximum FAR of 0.24 in APZ II
55	Retail trade – automotive, marine craft, aircraft, and accessories	N	Y	Y	Maximum FAR of 0.14 in APZ I & 0.28 in APZ II
56	Retail trade – apparel and accessories	N	N	Y	Maximum FAR of 0.28 in APZ II
57	Retail trade – furniture, home, furnishings and equipment	N	N	Y	Maximum FAR of 0.28 in APZ II



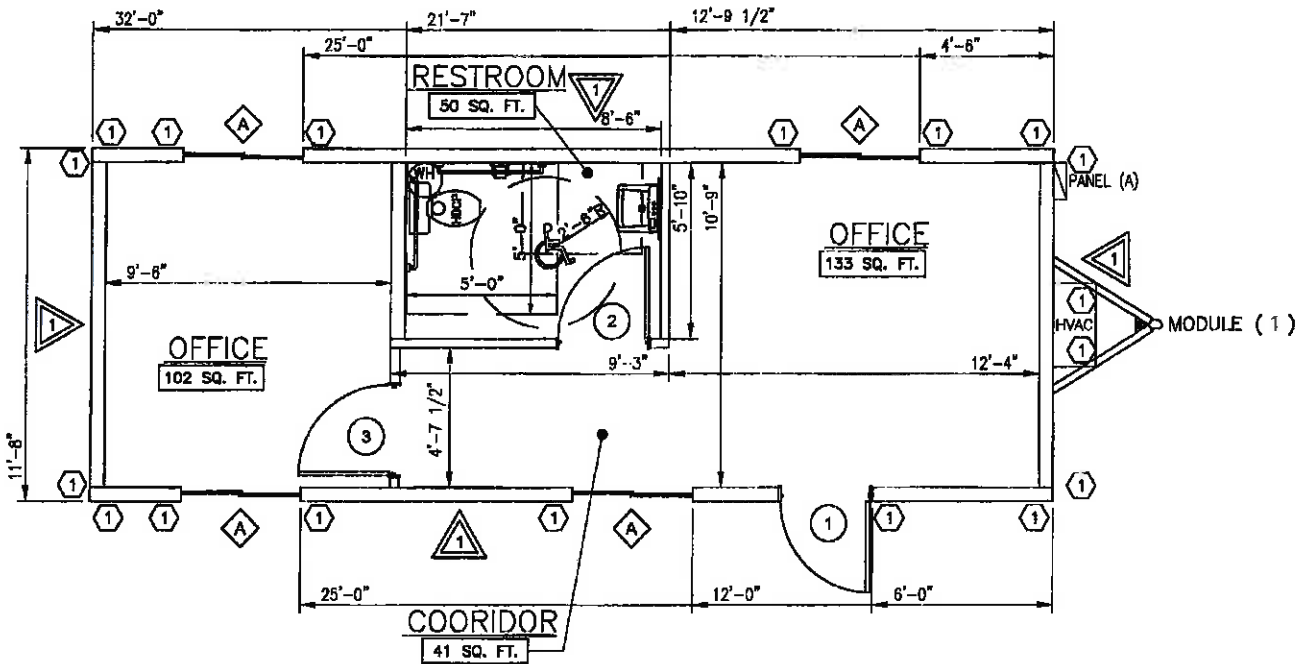
PREPARED BY:

BONADIMAN TEL: (909) 995-3800
REGISTERED ENGINEERS & ARCHITECTS INC.

ZONE CHANGE EXHIBIT
 FOR A.P.N. 294-180-063 & 294-170-010
 COUNTY OF RIVERSIDE, CA

DISPARD PRINTS BEARING EARLIER REVISION DATES → 12-05-17 SHEET 1 OF 1

A



STRUCTURAL LEGEND	
	SPECIAL TD (SEE STRUCTURAL FRAME FOR DESCRIPTION, SHEET 134)
	BUILD DOWN TD (SEE STRUCTURAL FRAME FOR DESCRIPTION, SHEET 134)

PLUMBING SCHEDULE		
SYMBOL	DESCRIPTION	QTY
	FLOOR DRAIN ELABORATED BELOW FOR THE HANDICAPPED BY STANDARD THROUGH HANDLE TO BE ON THE OPEN SIDE OF RESTROOM (25" RADIUS SPECIFIED) LOW FLOW	1
	ONE SINK GRAB BAR INCLUDES (1) 30" LONG & (1) 45" LONG. MOUNTED @ 34" AFF. (EXCLUDE 10" LONG GRAB BAR)	1 (SETUP)
	WALL MOUNT WATER HEATER (120V) EACH WATER HEATER TO INCLUDE: - INSULATION (2.5" MIN) ALL HOT WATER LINES - SEDIMENT STRIPS - EXPANSION TANK (MIN. 30" AFF.) - EXPOSED ON 18" MIN. WALLAGE SHELF BY 20"x40" W/4" BRACKETS (CROSS-SECTION ABOVE WATER CLOSET, BELOW RIG. SEE DETAIL 177)	1
	TOILET PAPER HOLDER (NO BRAND SPECIFIED)	1
	NO. 2000 SERIES 3 1/2" AFF. (NO BRAND SPECIFIED)	1

NOTE: 1. INCLUDE (2) RETROFIT BONES (1) DOOR (1) WALL
2. INCLUDE 3/4" W-LINE STRAPPING.

1 FLOOR PLAN
1/4"=1'-0" SQUARE FOOTAGE - 373 S.F.
OCCUPANCY LOAD = 14

PHOENIX MODULAR
SUNBELT MODULAR, INC.
14000 N. 28TH DR., BLDG. G, PHOENIX, AZ 85028
(602) 507-4700

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DEALER:
PACIFIC MOBILE
STRUCTURES

PROJECT:
STOCK BUILDING

STATES:
CALIFORNIA

SERIAL NUMBERS:
PMI-3888-1232
THRU
3892-1232

MODEL NUMBER:
PMI-PMS-1232-01

DRAWN BY:
VBBS

PLOT DATE:
2/25/2018

REV # / DATE:

APPROVED
FEB 27 2018
PPS CORPORATION

FEB 08 2018
LICENSED PROFESSIONAL ENGINEER
R. Mark Steele
C 35463
Exp. 09/30/2019
Civil Engineer
STATE OF CALIFORNIA

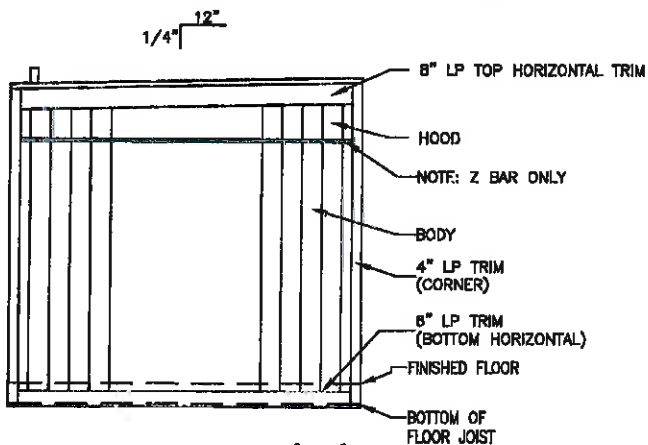
R. MARK STEELE, P.E.
CORPORATE ENGINEER
SUNBELT MODULAR, INC
11225 N. 28TH DR., BLDG. G, PHOENIX,
AZ 85028
(602) 507-4700

SHEET:

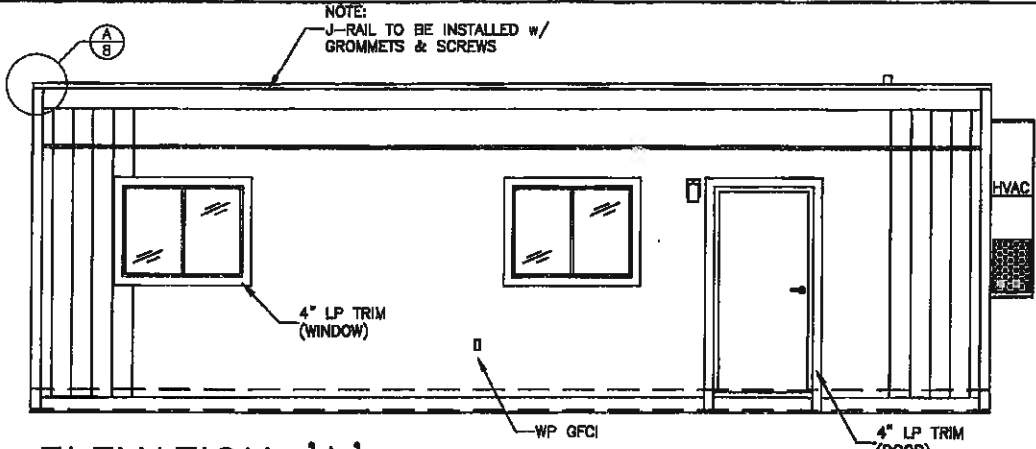
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DOORS:															
Qty	NO.	Type	Jamb	Wall Thickness	Glazing	Size	Rough Opening	U-Factor	Color	Clowner	Deadbolt	Hardware	Fire Rating	Keying	Notes
1	1	COMMERCIAL STEEL, INSULATED 18-GA.	FD 1824V DOORS, FINISH: GRAY	4 7/8"	NA	3688	37' X 81"	0.2	INTERIOR (PAVED MOBILE GRAY) EXTERIOR (PAVED MOBILE GRAY)	HYBRID TILT 125W TOP AND HALL (NO THIRD BOLTS)	SMALL CYLINDER (TYP. 020001)	PACKAGE LEVEL, GRADE 2 (TELL LOCKS) NO THIRD BOLTS	NONE	ALICE	DRIP CAP, (2) SL. STD. A (1) RUL. HSP. FINISH, BRASS/BRASS/BRASS. THRESHOLD, ODOOR SWEEP (INCLUDE RING; THIS DOOR TO REMAIN UNLOCKED WHEN IN USE IN OCCUPANCY)
1	2	PREPARED WOOD SOLID CORE	TRIPLE METAL JAMB BROOKSTONE	4 1/2"	NA	3298	57 1/2' X 81 1/4"	NA	LEADWOOD	NA	NONE	PENNY LEVER, GRADE 2 (TELL LOCKS) NO THIRD BOLTS	NONE	NA	FINISH A WALL MOUNT STOPS
1	3	PREPARED WOOD SOLID CORE	TRIPLE METAL JAMB BROOKSTONE	4 1/2"	NA	3088	57 1/2' X 81 1/4"	NA	LEADWOOD	NA	NONE	PACKAGE LEVEL, GRADE 2 (TELL LOCKS) NO THIRD BOLTS	NONE	NA	FINISH A WALL MOUNT STOPS

WINDOWS:									
Qty	NO.	Type	Glazing	Size	Rough Opening	U-Factor	SHGC	Mini-Blind	Notes
4	A	WATER WYLL HORIZONTAL BLIND	CLAR. DUAL GLAZED, LOWE	4830	47"X60"	.20	.23	NA	HEADERS @ 16" AFF GALVANIZED FLASHING SEE DETAIL 18



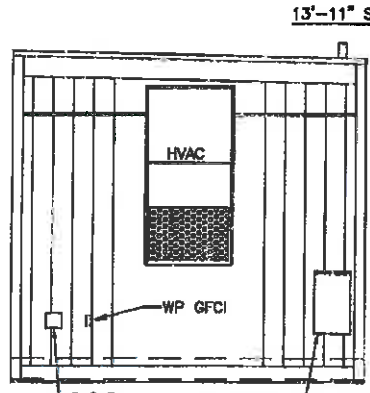
ELEVATION 'D'



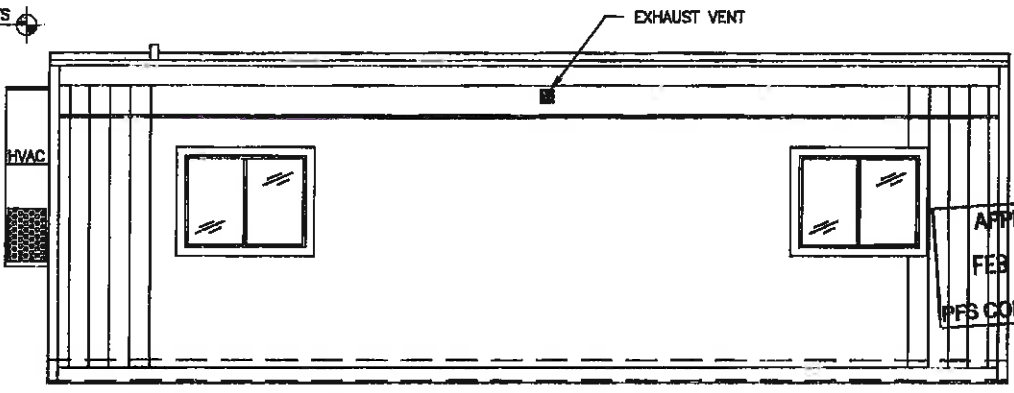
ELEVATION 'A'



NOTE:
DOOR TRIM AND CORNER TRIM TO OVERLAP
BOTTOM TRIM. CORNER TRIM MUST BE CAULKED.



ELEVATION 'C'



ELEVATION 'B'

APPROVED
FEB 27 2018
PFS CORPORATION

FEB 08 2018
LICENSED PROFESSIONAL ENGINEER
R. Mark Steele
C 35493
Exp. 09/30/2019
Civil Engineer
STATE OF CALIFORNIA

PHOENIX MODULAR
SERIES W. MALCOLM BL. - PHOENIX, AZ 85008
THE PHOENIX GROUP FAX: 602-944-4444

SUNBELT MODULAR, INC.
11001 N. 19TH DR., BLDG. G, #200
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ANYWAY WITHOUT
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PERMISSION.

DEALER:
PACIFIC MOBILE
STRUCTURES

PROJECT:
STOCK BUILDING

STATES:
CALIFORNIA

SERIAL NUMBERS:
PMI-3888-1232
THRU
3882-1232

MODEL NUMBER:
PMI-PMS-1232-01

DRAWN BY:
VB/BS

PLOT DATE:
2/5/2018

REV # / DATE:

R. MARK STEELE, P.E.
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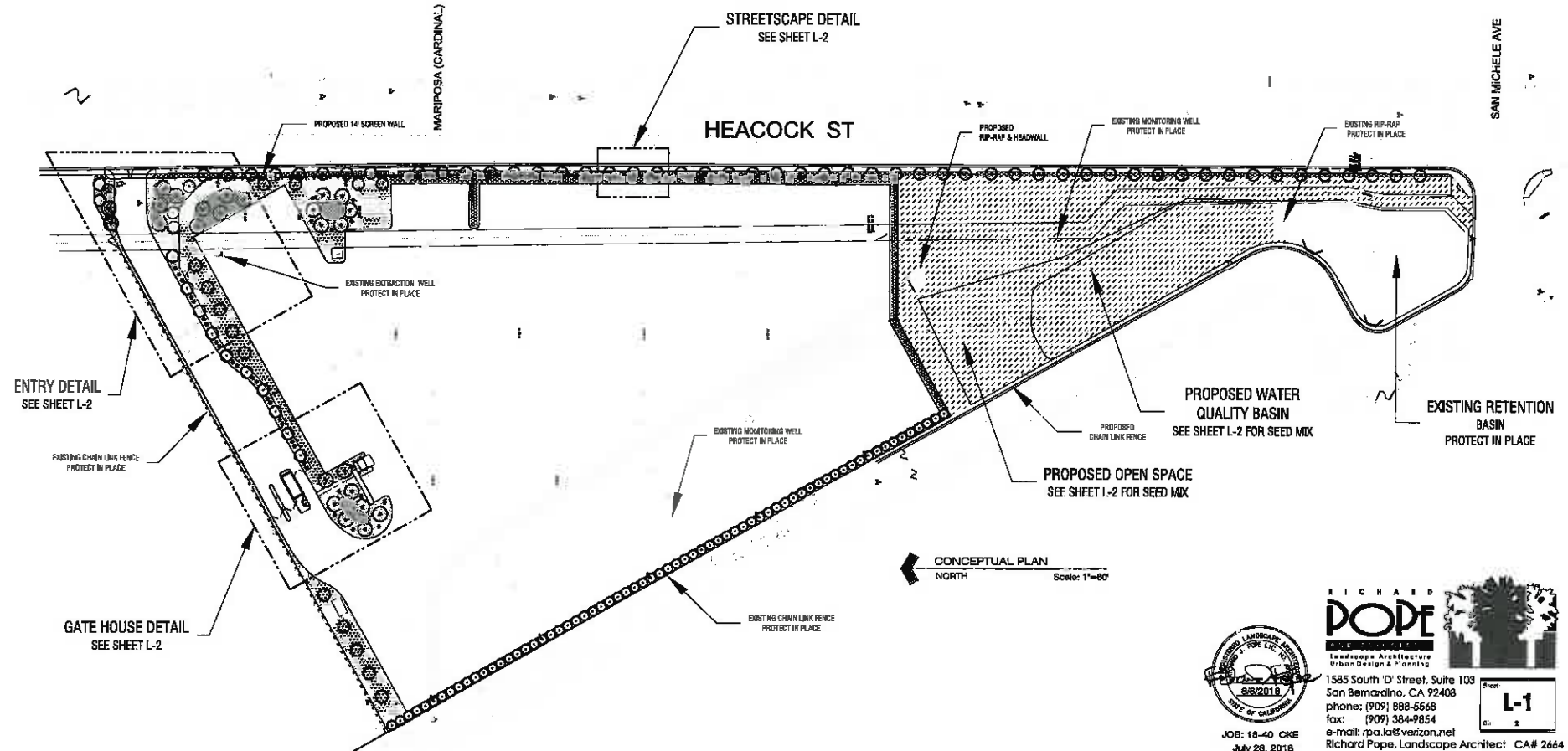
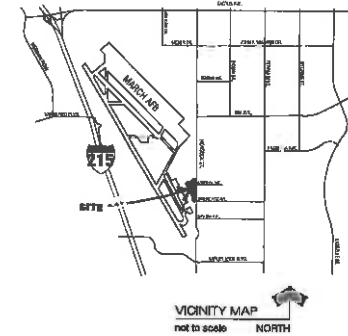
Heacock Street Truck Terminal

CONCEPTUAL LANDSCAPE PLAN

N.W.C. Heacock Street. & San Michele Ave.
Riverside County, CA

ON-SITE LANDSCAPE AREA: 62,463 SQ. FT.
STREET FRONTAGE: 10,109 SQ. FT.
BASIN/OPEN-SPACE AREA: 100,650 SQ. FT.
TOTAL LANDSCAPE AREA: 173,222 SQ. FT.

Applicant:
TRUCK TERMINAL PROPERTIES
1820 San Vicente Blvd.
Santa Monica, CA 90402
Contact: Bob Nassir
Phone (310) 466-7225
Email: bobnass5@gmail.com



RICHARD POPE
LANDSCAPE ARCHITECT
Landscape Architecture
Urban Design & Planning

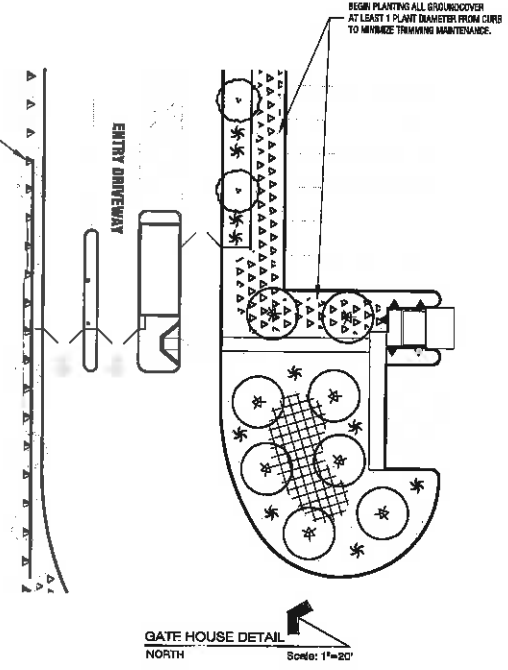
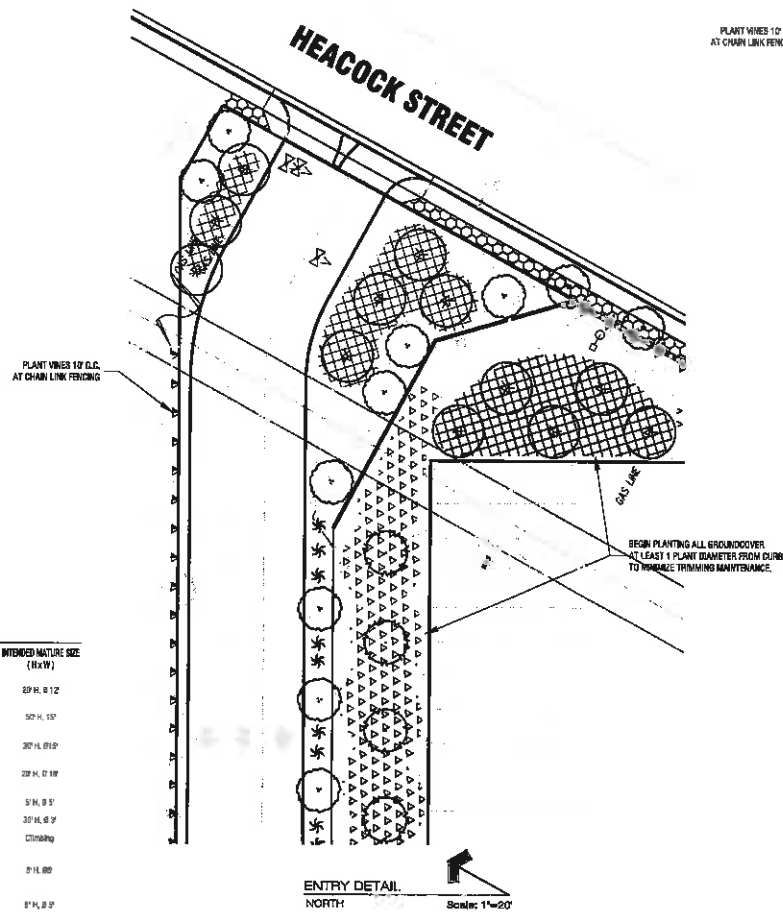
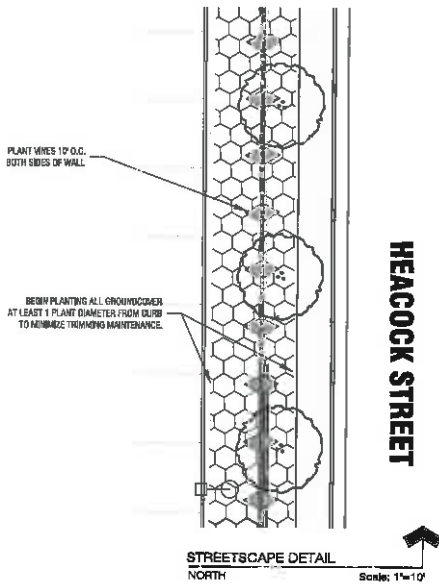
1585 South 'D' Street, Suite 103
San Bernardino, CA 92408
phone: (909) 888-5568
fax: (909) 384-9854
e-mail: rpa.la@verizon.net
Richard Pope, Landscape Architect CA# 2664

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of
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JOB: 18-40 CKE
July 23, 2018

Heacock Street Truck Terminal

CONCEPTUAL LANDSCAPE PLAN DETAILS



PLANTING LEGEND

SYMBOL	QTY.	SIZE	BOTANICAL NAME	COMMON NAME	WUCOLS #	INTENDED NATURE SIZE (HxW)
	67	16 Gal.	<i>Opuntia</i> ssp. <i>lybertyi</i>	England Cactus	Med	39 H, 8 1/2
	70	24" Box	<i>Ficus confertifolia</i>	Cowboy Island Pine	Med	50 H, 15'
	11	15 Gal.	<i>Melaleuca quincernaria</i>	Cajuput Tree	Mod	30'x18, 015'
	24	16 Gal.	<i>Podocarpus nerioides</i> 'Green Mallee' (bareless)	Pink Mallee Tree	Low	22 H, 0 1/2'
	22	3 Gal.	<i>Agave americana</i>	Century Plant	Low	5' H, 8 1/2'
	178	1 Gal./10' o.c.	<i>Ficus pumila</i>	Creeper Fig	Mod	30' H, 8 1/2'
	111	1 Gal./10' o.c.	<i>Ulex europaeus</i>	Carob Leguminosae	Mod	Climber
	13,222	1 Gal. 9" o.c.	<i>Acacia retinosa</i> 'New Bay'	Prostrate Acacia	Low	5' H, 8'
	17,679	1 Gal./10' o.c.	<i>Dodonaea viscosa</i>	Trailing Indigo Bush	Low	8' H, 8 1/2'
	5,862	1 Gal. 9" o.c.	<i>Yucca elata</i> 'New Gold' 2' High Mix	Compact Yucca Lantern	Low	2' H, 8 1/2'
	98,111	3/4" PL	5 & 5 NATIVE MIX - FREE MIX (various native plants)	Native Plants	Med	8x8 Seeds Captions, CA 800-884-0436 www.seedsolutions.com
	17,679	3/4" PL	3" Thick Layer Decomposed Granite	3" Thick Layer Decomposed Granite		

NOTES:

NO SUBSTITUTIONS, SPECIES OR VARIETY. CONTRACTOR ASSUMES ALL RESPONSIBILITY FOR ALL ISSUES RESULTING FROM SUBSTITUTIONS. DESIGN WAS CREATED USING EXACT VARIETY INDICATED.
 IF SUBSTITUTIONS ARE NECESSARY, CONTRACTOR MUST CONTACT LANDSCAPE ARCHITECT FOR SUBSTITUTION APPROVAL.
 A 3" DEEP LAYER OF SHREDDED BARK MULCH SHALL BE PLACED IN ALL PLANTING BEDS, EXCEPT LAWN AREAS.
 PLANT COUNTS ARE FOR BIDDING PURPOSES ONLY. CONTRACTOR SHALL BE RESPONSIBLE FOR EXACT COUNT PER PLAN.
 ALL ABOVE GROUND UTILITIES AND IRRIGATION EQUIPMENT SHALL BE SCREENED.



JOB: 18-40 CKE
July 23, 2018



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San Bernardino, CA 92408
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e-mail: r.papa@verizon.net

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of
2

Richard Pope, Landscape Architect CA# 2664

NOTICE OF PUBLIC HEARING RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Thursday from 8:00 a.m. to 5:00 p.m., except Monday, October 8 (Columbus Day), and by prescheduled appointment on Fridays, from 9:00 a.m. to 5:00 p.m.

ATTENTION: ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. The March Joint Powers Authority will hold hearings on this item and should be contacted on non-ALUC issues.

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside California

DATE OF HEARING: October 11, 2018

TIME OF HEARING: 9:30 A.M.

CASE DESCRIPTION:

ZAP1328MA18 – Truck Terminal Properties (Representative: Joseph E. Bonadiman & Associates, Inc.) – March Joint Powers Authority Case Nos. Change of Zone 17-01, Plot Plan 17-07. The applicant proposes to establish a truck parking terminal consisting of 258 truck trailer parking spaces (and 3 regular vehicle parking spaces) totaling 11.2 acres of paved parking area, a 450 square foot security office, and 0.36 acres of basin area, on a 13.68 acre site located westerly of Heacock Street, northerly of the westerly extension of San Michele Road, and easterly of the March Air Reserve Base. The applicant also proposes a change of zone for the site to Industrial. (Airport Compatibility Zone B2 of the March Air Reserve Base/Inland Port Airport Influence Area).

FURTHER INFORMATION: Contact ALUC Planner Paul Rull at (951) 955-6893. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Mr. Jeffrey Smith of the March Joint Powers Authority at (951) 656-7000.



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP1328MA18 DATE SUBMITTED: August 23, 2018

APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

March
Bd

Applicant	Truck Terminal Properties	Phone Number (310) 466-7225
Mailing Address	1820 San Vicente Blvd. Santa Monica, CA 90402	Email bobnass5@gmail.com
Representative	Joseph E. Bonadiman & Assoc., Inc	Phone Number 909-885-3806
Mailing Address	234 N. Arrowhead Avenue San Bernardino, CA 92408	Email ejb@bonadiman.com
Property Owner	March Inland Port Authority	Phone Number 951-656-7000
Mailing Address	23555 Meyer Drive Riverside, CA 92518	Email smith@marchjpa.com

LOCAL JURISDICTION AGENCY

Local Agency Name	March Joint Powers Authority	Phone Number 951-656-7000
Staff Contact	Jeff Smith	Email smith@marchjpa.com
Mailing Address	23555 Meyer Drive Riverside, CA 92518	Case Type
Local Agency Project No	Heacock Street Truck Terminal (PP 17-07) (C2.17-01)	<input type="checkbox"/> General Plan / Specific Plan Amendment <input type="checkbox"/> Zoning Ordinance Amendment <input type="checkbox"/> Subdivision Parcel Map / Tentative Tract <input checked="" type="checkbox"/> Use Permit <input type="checkbox"/> Site Plan Review/Plot Plan <input type="checkbox"/> Other

PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address	NORTHWEST OF HEACOCK STREET & SAN MICHELE ROAD INTERSECTION		
Assessor's Parcel No.	POR. A.P.N. 294-180-053 & 294-170-010	Gross Parcel Size	13.68
Subdivision Name		Nearest Airport	
Lot Number		and distance from Airport	March (on airport)

PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe)	Site is currently vacant. There is an existing basin used for water quality and storm drain mitigation. There are a few monitoring wells on site. There is also a sewer lift station on site.

Proposed Land Use (describe)	Develop approximately 11 acres of the existing site into a truck terminal and trailer parking facility per the attached site plan.		
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)		
For Other Land Uses	Hours of Operation	24 hours/day 7 days/week	
(See Appendix C)	Number of People on Site	Maximum Number	3 to 5
	Method of Calculation	Needed per shift.	
Height Data	Site Elevation (above mean sea level)	1,489	ft.
	Height of buildings or structures (from the ground)	10	ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No
	If yes, describe		

- A. **NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. **REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. **SUBMISSION PACKAGE:**
- ✓ 1. Completed ALUC Application Form
 - ✓ 1. ALUC fee payment
 - ✓ 1. Plans Package (24x36 folded) (site plans, floor plans, building elevations, landscaping plans, grading plans, subdivision maps)
 - ✓ 1. Plans Package (8.5x11) (site plans, floor plans, building elevations, landscaping plans, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
 - 1. CD with digital files of the plans (pdf)
 - ✓ 1. Vicinity Map (8.5x11)
 - ✓ 1. Detailed project description
 - 1. Local jurisdiction project transmittal
 - 3. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
 - 3. Gummed address labels of all surrounding property owners within a 300 foot radius of the project site (**only required if the project is scheduled for a public hearing Commission meeting**). If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10) with ALUC return address. *

* Projects involving heliports/helicopter landing sites will require additional noticing procedures.

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.7

HEARING DATE: October 11, 2018

CASE NUMBER: ZAP1274MA17 – Riverside Inland Development, LLC./Hillwood Investment Properties (Representative: Kathy Hoffer)

APPROVING JURISDICTION: March Joint Powers Authority

JURISDICTION CASE NOS: General Plan Amendment 16-01, Specific Plan 16-02, Plot Plan 16-02, Parcel Map 16-02 (Tentative Parcel Map No. 37220)

MAJOR ISSUES: Security, drainage, and potential for glare and storage of hazardous materials in close proximity to the military runway are major concerns. The Air Force has requested that these concerns be addressed in the project EIR, which has not yet been released.

RECOMMENDATION: Staff recommends, for reasons stated herein, that the Commission find the proposed General Plan Amendment, to expand the existing “Aviation” land use designation to incorporate general warehousing/logistics uses and the Specific Plan INCONSISTENT with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan. The proposed Tentative Parcel Map and Plot Plan must also be found INCONSISTENT, as these proposals require approval of the general plan amendment and specific plan.

Alternatively, should the Commission find the General Plan Amendment and Specific Plan consistent with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, both the parcel map and plot plan would also be found consistent.

PROJECT DESCRIPTION: The applicant proposes to construct two industrial buildings totaling 2,185,618 square feet on 142.5 acres. The applicant also proposes to amend the March Joint Powers Authority General Plan to include general warehousing/logistics uses as allowable land uses on lands designated as “Aviation” (AV), to amend the site’s designation from “AV” to “AV (SP-8)”, and to update the Building Capacity table in the Land Use Element. Specific Plan No. 16-02 is a proposal for a new Specific Plan (SP-8) that provides goals, policies, programs, land uses, development standards, and design guidelines for development on this site. Tentative Parcel Map No. 37220 proposes to divide the site into two parcels (one for each building).

PROJECT LOCATION: The site is located easterly of Interstate 215, southerly of March Air Force Base Museum and easterly terminus of Van Buren Boulevard, northerly of Nandina Avenue, and

westerly of March Air Reserve Base, within the jurisdiction of the March Joint Powers Authority, approximately 950 feet westerly of Runway 14-32 at March Air Reserve Base.

LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

- a. Airport Influence Area: March Air Reserve Base
- b. Land Use Policy: Zone B2
- c. Noise Levels: 65 – 75 range CNEL from aircraft

BACKGROUND:

General Plan Amendment and Specific Plan: California Public Utilities Code Section 21676 requires that the local jurisdiction refer any amendment of a general plan or specific plan within an Airport Land Use Compatibility Plan to ALUC for review. Therefore, it is within ALUC's purview to review the proposed general plan amendment and specific plan to ensure their long-term compatibility with the current and future operations at March Air Reserve Base.

The site's General Plan land use is currently designated as Aviation, which is described as:

Aviation (AV) (FAR 0.40)

Through joint use of the aviation field, the unique opportunity to develop civilian aviation is presented. Land uses under this designation include flightline, hangars, aviation support services, inclusive of fuel systems and dispensing, air cargo storage, passenger and air cargo terminals, fixed based operations, aircraft maintenance and aviation operation services. The maximum FAR for land use in this category is 0.40:1, with an average development intensity of 0.15:1.

The applicant proposes a General Plan Amendment that would amend the Aviation designation to include a Specific Plan Overlay that would allow for general warehousing/logistics uses on lands designated as "Aviation" by changing the second sentence to read as follows (shown in underlined text below):

Land uses under this designation include flightline, hangars, aviation support services, inclusive of fuel systems and dispensing, air cargo storage, passenger and air cargo terminals, fixed based operations, aircraft maintenance and aviation operation services as well as general warehousing/logistics uses.

Additionally, the proposed General Plan Amendment 16-01 would amend the site's designation from "AV" to "AV (SP-8)" and update the Building Capacity table in the Land Use Element.

At a macro-level, there are some concerns regarding the inclusion of general warehousing and logistics as allowable uses within the Aviation General Plan designation, essentially allowing for these uses to be located in close proximity to the runway. (The runway is approximately 1,003 feet from the project boundary and 1,383 feet from the proposed building.)

The designation of this site as “Aviation” on the March Joint Powers Authority’s General Plan predates the 2010 Joint Land Use Study and was expected to be maintained in the long term at the time that the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan was adopted. The proposed change to the Aviation designation to incorporate warehousing/logistics uses alters a foundational assumption in the design of Compatibility Zone B2.

One of the key project objectives of the proposed Specific Plan (#2.1.1) is to “facilitate the development of underutilized land currently planned for aviation-related uses that maximize the use of the site and responds to market demand within the VIP 215 Specific Plan area and surrounding region for a large format logistics center.” The proposed amendment raises unique concerns because the project site adjoins the west side of the primary runway at March Air Reserve Base. There are many other locations in Compatibility Zone B2 that would be available for a large format logistics center and would have less potential impact on the long-term viability of the Base.

While there is no question as to the integrity of the project applicant’s team, the presence of non-aviation/nonmilitary uses in large buildings in close proximity to the runway could impact the security of the Base. Hazardous materials (and other nefarious items) could potentially be stored within these buildings with little or no supervision from any governmental authority. However, if the project was granted taxiway access to the airport/Base and the buildings depicted on the official Airport Layout Plan, the operations therein could be inspected by Base staff.

The new Specific Plan provides a list of permitted uses:

- 3.2.2 Ancillary Uses. An ancillary use is a permitted use which is subordinate to the primary permitted use. The following are uses which are permitted within the Specific Plan area as ancillary uses in support of and subordinate to the primary permitted uses.
 - a) Cellular transition facilities and structures.
 - b) Offices, including corporate, subsidiary and regional management offices.
 - c) Maintenance facilities (internal) associated with a permitted use.
 - d) Showrooms and retail uses not to exceed 5% of the gross building area.
 - e) Outdoor vehicle, equipment and container storage ancillary to an approved use accommodated within an approved building (outdoor storage shall be screened when facing the public right-of-way).
 - f) Short-term construction yards.
 - g) Public utility uses and structures.
 - h) Employee support uses including cafeteria/café and training facilities as an ancillary use.
 - i) Uses determined by the Planning Director to be similar and not more intensive than

other allowed ancillary uses.

- 3.2.3 Conditional Uses subject to further Environmental Review.
 - a) Aviation Related Facilities
 - b) Facilities incorporating greater than 5% total building area in showroom/retail space.
 - c) Cold storage facilities larger than 10,000 square feet incorporating ammonia refrigeration or other refrigerants which are combustible or toxic.

None of these uses are prohibited within Compatibility Zone B2. However, the following uses could potentially accommodate levels of human occupancy that would exceed the maximum permissible average (100) and single acre (250) intensities for Compatibility Zone B2.

- 3.2.2 d) showrooms and retail uses not to exceed 5% of the gross building area;
- 3.2.2 h) employee support uses including cafeteria/café and training facilities; and
- 3.2.3 b) facilities incorporating greater than 5% total building area in showroom/retail space.

The proposed plot plan does not include any uses other than offices and manufacturing. The project has been conditioned that if more intense uses are proposed (such as a showroom, retail, training facilities, or dining areas), that project would have to be submitted to ALUC for review.

Non-Residential Average Land Use Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zone B2, which limits average intensity to 100 people per acre.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, the following rates were used to calculate the occupancy for the proposed buildings in Compatibility Zone B2:

- Office – 1 person per 200 square feet (with 50% reduction)
- Manufacturing – 1 person per 200 square feet

The project proposes two industrial buildings totaling 2,185,618 square feet, which includes 40,000 square feet of office area and 2,145,618 square feet of manufacturing area, potentially accommodating an occupancy of 10,928 people, which would result in an average intensity of 77 people, which would be consistent with the Compatibility Zone B2 criterion of 100.

Considering average intensity on a lot-by-lot basis (based on the applicant's proposed parcel lines as indicated on the site plan and parcel map):

- Building 1 lot (64.87 acres) consisting of 15,000 square feet of office area and 999,822 square feet of manufacturing area would accommodate 5,074 people, resulting in an average intensity of 74 people per acre,
- Building 2 lot (77.59 acres) consisting of 25,000 square feet of office area and 1,145,796 square feet of manufacturing area would accommodate 5,854 people, resulting in an average

intensity of 75 people per acre.

Both lots are consistent with the Compatibility Zone B2 average criterion of 100.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle and 1.0 persons per truck trailer parking in the absence of more precise data). Based on the 895 parking stalls and 945 truck trailer stalls provided, the total occupancy would be estimated to be 2,288 people. The resulting average intensity of 16 people per acre is consistent with the Compatibility Zone B2 average criterion of 100.

Non-Residential Single-Acre Land Use Intensity: Compatibility Zone B2 limits maximum single-acre intensity to 250 people. There are no risk-reduction design bonuses available, as March Air Reserve Base/Inland Port Airport is primarily utilized by large aircraft weighing more than 12,500 pounds.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre area would be located in either building and consist of 15,000 square feet of office area and 28,560 square feet of manufacturing area, accommodating 218 people, which is consistent with the Compatibility Zone B2 single acre criterion of 250.

March Air Reserve Base/United States Air Force Input:

The MARB staff previously reviewed the project and provided a comment letter dated September 27, 2016, which identified several issues including security, drainage, glare, and safety.

ALUC staff prepared a comment letter dated August 23, 2018, requesting that the applicant address the comments provided by MARB staff, as well as an additional comment from ALUC staff regarding the site's capability to be used as an aviation land use.

The applicant provided a response to comment letter dated September 14, 2018, which addresses the comments raised by MARB and ALUC staff (most items are addressed in the proposed Specific Plan and design drawings), specifically:

1. Security. Cameras be facing away from the runway and must not record airfield operations. At the request of the MARB staff, a chain link fence with barbed wire shall be installed on the easterly perimeter to allow for ease of viewing activities of the project tenants. The project will not impede the MARB's mission to carry out Anti-Terrorism/Force Protection procedures or maneuvers. The applicant has agreed (in general) to incorporate within the tenant lease agreement specific language that will allow MARB and/or U.S. Air Force Security forces to access the property during an imminent threat, while providing reasonable notice to the tenant absent an imminent threat.
2. Drainage. The applicant has had multiple meetings and correspondences with MARB and March Joint Powers Authority staff regarding the project's proposed drainage solution. A

detailed analysis of the offsite drainage was prepared by the applicant’s engineer which identified that downstream floodplain impacts are anticipated to be less than historical flow and existing condition due to the improvements proposed by the project.

3. Glare. The proposed Specific Plan indicates that materials shall be of a non-reflective material and that highly reflective materials facing the runway or aircraft flight path are prohibited. The applicant has indicated that no rooftop solar panels are proposed.
4. Safety. The proposed Specific Plan prohibits hazardous materials such as above ground petroleum storage containers and below ground storage containers in excess of 10,000 gallons. The project does not propose any hazardous materials at this time.
5. Aviation Land Use Designation Capability. The applicant has indicated that the project will not obstruct future aviation use or the right to obtain taxiway access to the runway from the project site.

Given that the project site is located in Zone B2 westerly of the runway at March Air Reserve Base (MARB), the MARB staff was notified that the project it had commented on two years ago was back on the calendar and the updated project package of plans was sent for their review. MARB staff provided a comment letter dated September 17, 2018, renewed the concerns referenced in their previous comments letter dated September 27, 2016. It also added: the intention of the MARB staff to continue to “monitor and make appropriate responses as it progresses through” the CEQA processes, urged that stormwater systems “not negatively impact the installation or operations”, requested additional mitigation measures, looking forward to notice of future hearing and “being involved as the application progresses through the MJPA”.

The issues identified in the Air Force comment letter, the ALUC comment letter, and the applicant’s response letter are discussed in the following table.

Issues	Air Force	ALUC	Applicant
Security	Cameras shall not face or record any actions or portion of the base operations. Perimeter fencing shall be constructed out of block.	In addition to the Air Force comment regarding security, ALUC staff raised the concern of the project’s size and proximity to the runway with regards to the Base’s Anti-Terrorism Force Protection and their ability to carry out its mission of protecting the base. Specifically, since the project expands the use of the site to include non-aviation use, ALUC staff is concerned with the ability of Base security personnel to immediately respond to a threat at the project	No cameras will be oriented towards the runway and cameras will not record base operations. Fencing along the project-airport boundary shall be minimum 8 feet in height with three strands of barbed wire, and shall be of a durable material subject to the MJPA and Base review. This type of fencing was requested by the Base Security Forces. The project will not impede Base’s mission to carry out Anti-Terrorism Force Protection procedures. The applicant will work with their legal counsel to craft the appropriate commercially reasonable language for lease agreements regarding Base security

		<p>facility. Therefore it is recommended that the master lease and sublease include provisions that enable Base security personnel to respond immediately to what they perceive as a possible risk at the project facility.</p> <p>Recommended condition is included that states “The U.S. Air Force has the right and authority, in its sole discretion, to inspect the premises without prior notice for the security of its operations and personnel”.</p>	<p>forces to access the property during an imminent threat (same access as provided to law enforcement and emergency response teams) while providing reasonable notice to tenants absent an imminent threat.</p> <p>No language has been provided yet.</p>
Drainage	<p>Rising groundwater table at the base is an on-going concern, specifically, the ability of a project to drain water detention basins within 48 hours. Base staff shall review basin design. These basins shall address Bird Wildlife Aircraft Strike Hazard concerns.</p>	<p>In addition to the Air Force comment regarding drainage, ALUC staff supports the Base’s request to review all drainage plans prior to approval.</p>	<p>The proposed Specific Plan identifies how the project will address storm water drainage to be consistent with NPDES and WQMP requirements. The applicant has met with MJPA and Base staff in discussing proposed drainage solutions. Applicant’s drainage studies indicates the downstream floodplain limits are less (than historical flow and current condition) due to the project’s improvements.</p> <p>No drainage solution has been agreed upon yet (however, this will most likely occur during the Base’s review of the project’s EIR).</p>
Glare	<p>Solar panels or any reflective materials on</p>	<p>In addition to the Air Force comment regarding glare, ALUC staff notes that the project does not propose solar</p>	<p>The proposed Specific Plan indicates that materials shall be of a non-reflective material, and that highly reflective materials on elevations facing the</p>

	the rooftop are prohibited. Construction material shall be non-reflective including outside ductwork, windows, and roofs.	panels at this time. Any future solar panels could potentially result in significant glare impacts, and therefore, a solar glare hazard analysis would be required to analyze the impacts. In the event of any reasonable complaint about glare related to aircraft operations, the applicant shall agree to such specific mitigation measures as determined or requested by MARB.	runway or aircraft approach path are prohibited. Solar panels are prohibited.
Safety	No hazardous materials shall be stored within the facility.	No additional comments to the Air Force Comment regarding safety.	The proposed Specific Plan prohibits above ground petroleum storage containers and below ground storage containers in excess of 10,000 gallons.
Aviation Land Use	Not identified.	The project expands the permissible use of the property from aviation only to include non-aviation uses. The applicant shall agree to conditions being placed on the project that will preserve the ability for subsequent aviation use and to construct taxiways and access to the runways. Recommended condition is included that states “the project shall not pre-empt future opportunities for the extension of taxiway access to the runway from the site”.	The proposed Specific Plan identifies the site as un-zoned. The establishment of the Specific Plan will provide a designation on the MIPA zoning map with an underlying Aviation Designation. Under the proposed Specific Plan, light manufacturing and assembly uses including aviation related manufacturing is a permitted use. One of the key project objectives identified in the proposed Specific Plan is to “facilitate the development of underutilized land currently planned for aviation-related uses that maximize the use of the site and responds to market demand within the Specific Plan area and surrounding region for a large format logistics center.” The project will not obstruct future aviation use or the right to obtain taxiway access to the runway from the project.

It should be noted that the above issues will be considered, among others, in the upcoming Joint Land Use Study (JLUS) requested by the Air Force and supported by the March JPA and other local jurisdictions. (Air Force letter dated June 1, 2018, included in this staff report package.)

Due to the project's size and close proximity to the runway, it could potentially impact the JLUS review process or ability to implement its conclusions. As an example, the JLUS will look at increased glare from passive reflective roof surfaces, such as the two million square feet of roof surface on this proposed project.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zone B2.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being in an area within the 65-75 CNEL range from aircraft noise. As a primarily industrial use not sensitive to noise (and considering typical anticipated building construction noise attenuation of approximately 20 dBA), the warehouse area would not require special measures to mitigate aircraft-generated noise. However, a condition is included to provide for adequate noise attenuation within office areas of the buildings.

Part 77: The elevation of Runway 14-32 at its southerly terminus is 1,488 feet above mean sea level (1,488 feet AMSL). At a distance of approximately 950 feet from the runway to the closest parcel within the site, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,497.5 feet AMSL. The site is approximately 1,508 feet AMSL. With a maximum building height of 48 feet, the top point elevation would be 1,556 feet AMSL. Therefore, review of buildings by the FAA Obstruction Evaluation Service was required. Submittal to the FAAOES was made and Aeronautical Study Numbers 2016-AWP-12028 thru 12036-OE were assigned. "Determination of No Hazard to Air Navigation" letters were issued by the FAAOES dated January 26, 2017, and the studies revealed that the project's structures would not exceed obstruction standards and would not be a hazard to air navigation provided conditions are met. These FAAOES conditions will be incorporated into the project's conditions. Extensions of time have been granted for each of these determinations.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

CONDITIONS (if the project is found consistent):

1. Any outdoor lighting installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, congregate care facilities, hotels/motels, restaurants, places of assembly (including churches and theaters), buildings with more than 3 aboveground habitable floors, noise sensitive outdoor nonresidential uses, critical community infrastructure facilities and hazards to flight.
3. Prior to issuance of any building permits, the landowner shall convey and have recorded an avigation easement to the March Inland Port Airport Authority. Contact March Joint Powers Authority at (951) 656-7000 for additional information.
4. The attached notice shall be provided to all prospective purchasers of the property and tenants or lessees of the building.

5. The proposed detention basins on the site (including water quality management basins) shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping. Trees shall be spaced to avoid creation of a continuous canopy. Landscaping in and around the detention basin shall not include vegetation that produces seeds, fruits, or berries.
6. March Air Reserve Base personnel must be transmitted for their review and approval details of the storm water conveyance system and landscaping plans.
7. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
8. This project has been evaluated for 40,000 square feet of office area and 2,145,618 square feet of manufacturing area. Any proposals for showrooms, retail trade, and/or employee support uses such as cafeterias, training facilities, exercise rooms, or conference rooms, or any changes to the interior floor layout plan shall require subsequent review by the Airport Land Use Commission. In addition, this project shall not store, process or manufacture hazardous materials without review and approval by the Airport Land Use Commission.
9. Noise attenuation measures shall be incorporated into the design of the office areas of the proposed buildings, to the extent such measures are necessary to ensure that interior noise levels from aircraft operations are at or below 45 CNEL.
10. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission and March Air Reserve Base. In the event of any reasonable complaint about glare related to aircraft operations, the applicant shall agree to such specific mitigation measures as determined or requested by MARB.
11. The lease between the March Joint Powers Authority and the applicant (or any future tenants) shall include that the U.S. Air Force has the right and authority to inspect the premises without prior notice as needed for security of its operations and personnel in its sole discretion.
12. The project shall not pre-empt future opportunities for the extension of taxiway access to the runway from the site.

13. Any roof-top equipment or change in height that exceeds a total height of 48 feet will require Form 7460-1 submittal, review, and issuance of a “Determination of No Hazard to Air Navigation” by the Federal Aviation Administration Obstruction Evaluation Service.
14. The Federal Aviation Administration has conducted aeronautical studies of the proposed project (Aeronautical Study Nos. 2016-AWP-12028 thru 12036-OE) and has determined that the northeast and southeast corners of both buildings shall be lighted in accordance with FAA Advisory circular 70/7460-1 L Change 1, Obstruction Marking and Lighting, red lights – Chapter 4, 5 (Red), and 12.
15. Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as normal operation is restored, notify the same number.
16. The proposed structures shall not exceed a height of 48 feet above ground level. The maximum elevation at top point (including all roof-mounted equipment, if any) shall not exceed 1556 feet above mean sea level (AMSL) for Building 2 southeast and southwest corners, 1564 feet AMSL for Building 2 northeast and northwest corners, 1563 feet AMSL for Building 1 southeast and southwest corners, and 1571 feet AMSL for Building 1 northwest and northeast corners.
17. The maximum height and top point elevations specified above shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in structure height or elevation shall not require further review by the Airport Land Use Commission.
18. Temporary construction equipment used during actual construction of the structures shall not exceed 48 feet in height and a maximum elevation of 1571 feet above mean sea level, unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.
19. Within five (5) days after construction reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned or a decision is made not to construct the structure.



AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

August 23, 2018

Mr. Jeffrey Smith, Senior Planner
March Joint Powers Authority
14205 Meridian Parkway, Suite 140
Riverside CA 92518

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**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW
COMMENTS: VETERANS INDUSTRIAL PROJECT (VIP 215)**

File No.: ZAP1274MA17
Related File No.: General Plan Amendment 16-01, Specific Plan 16-02, Plot Plan, 16-02, Parcel Map 16-02 (Tentative Parcel Map No. 37220)

Dear Mr. Smith:

The Airport Land Use Commission (ALUC) staff has initially reviewed the March Joint Powers Authority case; Veterans Industrial Project 215 (known as the project), a proposal to construct two industrial buildings totaling 2,185,618 square feet on 142.5 acres, and amending the General Plan to allow for a "Specific Plan" overlay land use designation; a new Specific Plan document that provides goals, policies, programs, land uses, development standards, design guidelines and a tentative parcel map to create two legal parcels (one for each building).

STAFF

Director
Simon A. Housman

John Guerin
Paul Rull
Barbara Santos

The purpose of the ALUC is to protect people from airports and airports from people. The proposed project raises unique concerns because it is located adjoining the west side of the MARB primary runway. ALUC staff has identified several concerns regarding the project many of which are also outlined in the March Air Reserve Base letter dated September 27, 2016

County Administrative Center
4080 Lerron St., 14th Floor
Riverside, CA 92501
(951) 955-6132

Staff understands that the applicant and MARB have cooperated to address the MARB concerns. The purpose of this letter is to confirm they are satisfied with the agreed upon measures and identify additional concerns of the ALUC staff. One way of doing this would be to provide the ALUC staff with correspondence resolving the issues which could be referenced in the staff report to the ALUC. The issues are as follows (Base comments in **Red**):

www.aluc.org

1. Security.
 - a. Cameras not facing or recording any actions of Base operations. Require perimeter block wall to eliminate lighting and security concerns.
 - b. Anti-Terrorism/Force Protection and the ability of the base to carry on its mission are a significant concern for ALUC at MARB.
 - c. Specifically, since the proposed project expands the use of the property to include non-aviation use, the ALUC is concerned about the ability of the MARB security personnel to immediately respond if there is a threat to the base. The master lease and subleases should include a provision that enables MARB security forces to respond immediately to what they perceive as a possible risk at the project

AIRPORT LAND USE COMMISSION

facility. A condition will be included to allow MARB security to access the property and buildings. The specific language of the condition and provision should be commented on by the applicant and MARB security e.g. "The U.S. Air Force has the right and authority, in its sole discretion, to inspect the premises without prior notice for the security of its operations and personnel".

2. Drainage.

- a. Property is located in an area with rising groundwater table and MARB has concerns with the project draining the water detention basins within 48 hours.
- b. Base is requesting basins be oversized or covered in design to address future concerns. The MARB has also requested the project no rely on the degraded natural channels for storm water discharge. Rather "this development be connected to a larger regional storm water effort to route storm water around MARB completely. (Emphasis added.)
- c. The applicant shall provide correspondence with the MARB on how these issues have been resolved. The project will be conditioned with these mitigation measures.

3. Glare.

- a. All construction materials be of non-reflective material including outside ductwork, windows and roofs.
- b. The project shall not create: distracting lights, sources of dust, steam or smoke, or electrical interference with aircraft communications or navigation.
- c. Solar panels or any reflective materials on rooftop are prohibited.
- d. In the event of any reasonable complaint about glare related to aircraft operations, the applicant shall agree to such specific mitigation measures as determined or requested by MARB.

4. Safety.

- a. No hazardous materials stored within the facility.

5. Aviation Land Use Capability and Designation.

- a. The project expands the permissible use of the property from aviation only to include non-aviation uses. The applicant shall agree to conditions being placed on the project that will preserve the ability for subsequent aviation use and to construct taxiways and access to the runways, e.g. "That no lease, approval, or entitlement will obstruct the future aviation use and right to obtain taxiway access to the runway from the property".

These concerns and others will be addressed in the contemplated Joint Land Use Study (JLUS). It may be best to delay pursuing this particular project, which adjoins the primary runway, until that JLUS can be completed.

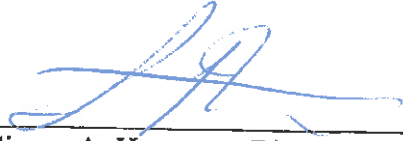
Alternatively, the applicant and MARB are requested to comment on and address all these issues raised by ALUC and MARB before the ALUC public hearing. It appears the applicant has addressed several of these concerns. ALUC's staff needs to confirm that MARB is satisfied with the mitigation measures. If not, what additional measures are necessary?

AIRPORT LAND USE COMMISSION

If you have any questions, please feel free to contact me at (760) 328-7995, or Paul Rull, ALUC Urban Regional Planner IV, at (951) 955-6893.

Sincerely,

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Simon A. Housman, Director

Attachments: March Air Reserve Base comment letter dated 9/27/16

**cc: Kathy Hoffer, Hillwood
Gary Gosliga, Airport Manager, March Inland Port Airport Authority
Sean P. Feeley, Base Civil Engineer, March Air Reserve Base
Daniel Rockholt, March Air Reserve Base
ALUC Case File**

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DEPARTMENT OF THE AIR FORCE
AIR FORCE RESERVE COMMAND

17 September 2018

MEMORANDUM FOR RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
ATTN: PAUL RULL, URBAN REGIONAL PLANNER IV
COUNTY ADMINISTRATIVE CENTER
4080 LEMON STREET, 14TH FLOOR
RIVERSIDE CA 92501

FROM: 452d Mission Support Group/Civil Engineers
Base Operating Support
610 Meyer Drive, Bldg. 2403
March ARB CA 92518-2166

SUBJECT: VIP 215, ZAP1274MA17 (Related MJPA applications GPA16-01; SP16-02; PP16-02; PM16-02/TPM No. 37220)

1. March Air Reserve Base (MARB) personnel have reviewed the proposal to construct two industrial buildings totaling 2,185,618 square feet on 142.5 acres, and amending the MJPA General Plan to allow for a Specific Plan overlay land use designation and associated entitlements, and offer the following comments:
2. MARB personnel provided March Joint Powers Authority (MJPA) personnel a letter in response to their Notice of Preparation for an Environmental Impact Report (EIR) for the subject project on September 27, 2016 (attached for reference). Please refer to that letter as our comments for this project application.
3. MARB personnel understands that this project will continue through the MJPA review and entitlement process, and that a Draft EIR will be circulated for review in the coming months. We will continue to monitor the project and make appropriate responses as it progresses through the California Environmental Quality (CEQA) and land use processes.
4. Additionally, mention of proper design of stormwater management systems and landscaping was included in our September 27, 2016 letter. However we would like to emphasize particular attention be made towards the design on drainage and detention basins for the project, and that such designs do not negatively impact the installation or operations. MARB personnel requests that details of the stormwater conveyance system and landscaping plans be reviewed and approved when those become available.
7. MARB personnel's position is the development is compatible with guidance found in AFI 32-7063, dated 18 December 2015, titled Air Installations Compatible Use Zones Program. The

following are some mitigation steps that could and should be implemented, and it would be our desire that all such steps are researched and implemented.

- BMPs should not introduce hazards related to birdstrikes.
- Properly drain stormwater detention basins within 48 hours.
- Noise level hazards should be mitigated.

8. We look forward to receiving notification of the final site plans, public hearing notices and public hearing minutes, and being involved as the application progresses through the MJPA entitlement process.

9. Thank you for the opportunity to review and comment on this proposed development. If you have questions, please contact Mr. Daniel “Rock” Rockholt at (951) 655-2236.

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SEAN P. FEELEY
Base Civil Engineer



DEPARTMENT OF THE AIR FORCE
AIR FORCE RESERVE COMMAND

27 September 2016

MEMORANDUM FOR MARCH JOINT POWERS AUTHORITY
ATTN: JEFFREY SMITH, SENIOR PLANNER
14205 MERIDIAN PARKWAY, SUITE 140
RIVERSIDE, CA 92518

FROM: 452d Mission Support Group/Civil Engineers
Base Operating Support
610 Meyer Drive, Bldg. 2403
March ARB CA 92518-2166

SUBJECT: Veterans Industrial Park 215 (VIP 215), Project Draft Environmental Impact Report (EIR).
The Assessor Parcel Numbers (APNs) are: 294-150-009, 294-170-005, 295-300-008, and 294-180-038

1. The March Air Reserve Base (MARB) review of the proposal to establish two warehouse facilities on 142.5 acres located directly southeast of the Interstate 215 Freeway off-ramp at Van Buren Boulevard, south of the March Field Air Museum, and west of March Air Reserve Base airport runway, is provided in this memorandum.
2. The parcel also known as Hillwood D2. This property is not located within any of the MARB Accident Potential Zones or Clear Zone for Runway 14-32. While the proposed use may be consistent with the Riverside County Airport Land Use Compatibility Plan (RCALUC) zoning and land use guidelines, the proposed project presents a concern due to the location being directly along the west side of the MARB primary runway. Buildings in this area should not be used for high-density functions since the objective of the land use guidelines in and around APZ's is to restrict people-intensive uses due to a greater risk of aircraft incident in these areas. There are restrictions on land uses and heights of natural objects and man-made objects in the vicinity of air installations that may obstruct the airspace, attract birds, cause electromagnetic or thermal interference, or produce dust, steam, smoke, or light emissions to provide for safety of flight and the public welfare.
3. Due to the proximity of MARB, we request that any cameras are not facing or recording any actions or portion of the base/airfield operations and perimeter fencing of the facility be constructed of block walls to help eliminate lighting and security concerns.
4. The parcel is partially located within the Perris North sub-basin, the same groundwater sub-basin as MARB. The rising groundwater table at MARB is an ongoing concern and solutions are being sought by both MARB and the State of California. MARB has concerns with the rising groundwater and the ability to properly drain the water detention basins within 48 hours. Given the depth of current ground water we are concerned that proposed basins may fill with ground water, depending on the depth, and pumping ground water into storm water systems is not permitted. In the event that the basins no longer drain as originally designed, MARB is requesting the water detention basins be oversized to address future concerns or be placed underground or covered. Prior to issuance of formal approval, MARB requests to see specific designs of basins and if uncovered, calculations that show sufficient additional capacity if rock is added later.

5. A properly designed storm water management system and landscaping must address Bird/Wildlife Aircraft Strike Hazard (BASH) concerns including proper detention/infiltration of storm water runoff. In addition we are keenly concerned with the proposed runoff intended to discharge through existing degraded natural channels on the base property. We request that this development be connected to a larger regional storm water effort to route storm water around MARB completely. Any new drainage (even at historical levels) onto the base, further degrades our natural infrastructure, increases discharge period and creates potential ponding on the airfield. Further, MARB is concerned with runoff from new development impacting our monitoring requirements and NPDES permit on the south of the base where the storm water exits. The base will want to review details of the storm water conveyance system and the landscaping plan when they become available. Given the proximity to the airfield, trees which will bear mast or grow to an adequate size for roosting should not be planted. Additional information on reducing BASH hazards can be found in AFPAM 91-212, *Bird Wildlife Aircraft Strike Hazard (BASH) Management Techniques*, dated February 1, 2004. We request that March Joint Powers Authority evaluate the storm water detention basin design to mitigate or eliminate any hazards, and jointly approve the design with MARB.

6. The site is located within an area that is exposed to elevated levels of noise from the base's flying operations: reference the AICUZ for MARB for the noise contours for this area. A more recent model can be found within the Land Use Compatibility Plan (LUCP) adopted by the Riverside County Airport Land Use Commission (ALUC) dated November 13, 2014. The document titled, "Background Data: March Air Reserve Base/Inland Port Airport Environs," is an insert to the LUCP. Referencing Exhibit MA-4 from the aforementioned insert, it would appear this project resides within an area that is subject to 75 dB Community Noise Equivalent Level (CNEL). In addition, this site is situated in an area where aircraft flying departures and arrivals from Runway 14/32 will overfly this area at relatively low altitudes. In addition, these aircraft will be at a high power setting generating significant noise contours. As such, the employees of this proposed development would be subject to noise occurrences that may generate complaints to the base and the community leadership. Employees and regularly received public may require protection using noise abatement (noise level reduction) provided in the construction design. Additionally, hearing protection for employees may be required by Occupational Safety and Health Administration (OSHA) or other agencies as it relates to safety and health in a high noise work environment.

7. To help eliminate any potential adverse effects on aircraft operations at MARB, we ask that materials provided in construction be of a non-reflective material such as outside ductwork, windows and roofs by means such as painting or covering. In addition, none of the project improvements shall create:

- Distracting lights which could be mistaken for airport lights
- Sources of dust, steam, or smoke which may impair pilot visibility
- Sources of electronic interference with aircraft communications or navigation

8. The following are some mitigating measures and it is our desire that all such steps be researched and implemented.

- BMPs must be reviewed by the Base and must not introduce hazards related to birdstrikes
- Solar panels or any reflective materials on the rooftop are prohibited
- No hazardous materials should be stored within this facility due to the proximity of the Airfield
- Noise level hazards must be mitigated

9. Thank you for the opportunity to review and comment on this proposed development. If you have questions please contact Ms. Denise Hauser at (951) 655-1862.



SEAN P. FEELEY
Base Civil Engineer



September 14, 2018

Via Email and overnight Delivery

Mr. Simon A. Housman
Director
Airport Land Use Commission, Riverside County
4080 Lemon Street, 14th Floor
Riverside, CA 92501
shousman@rivco.org

Subject: Airport Land Use Commission Development Review Comments letter dated August 23, 2018, regarding Veterans Industrial Project 215 (VIP 215)

Dear Mr. Housman:

Hillwood, A Perot Company, is partnering with the March Joint Power Authority to develop the 142 acres of land known as the Veterans Industrial Project 215. We are in receipt of your letter of August 23, 2018 and have been asked by RCALUC to respond to your letter.

Most of the items mentioned in your letter are addressed in the Veterans Industrial Park 215 Specific Plan (“VIP 215 Specific Plan”) and reflected accordingly in the design drawings submitted to the March Joint Powers Authority. As stated in the VIP 215 Specific Plan, item 2.1.3 - Land Use Compatibility, and item 3.1.9 – March Air Reserve Base Performance Standards:

The Specific Plan Area’s location adjacent to the runway area subjects the site to a number of unique conditions which are reflected in the project’s development regulations and site design, including:

- ***Height limitations in the 7:1 surface and a building restriction line for structures of over 35 feet;***
- ***Security fencing along the runway property and a clear area beyond;***
- ***Limitations on standing water; and***
- ***Limitations on reflective surfaces facing the runway.***

The Veterans Industrial Park 215 project is located in MARB Airport Influence Zone, and all development within the plan shall comply with measures of

- 1) Avigation Easement;***
- 2) Noise Standard;***
- 3) Retention and Water Quality Basin;***
- 4) Lighting Plan; and***
- 5) Height Restrictions per Federal Aviation Regulations Part 77.***

Further, in the VIP 215 Specific Plan, under the General Plan Consistency Table, Land Use Goal 7; Transportation Policy 2.6; Resource Management Policy 9.8 and Safety/Risk Management Goal 7:

The project is designed to incorporate appropriate uses as defined in the Air Installation Compatible Use Zone (AICUZ) Study, Airport Layout Plan, Department of Defense Instructions and Air Force Instructions. The project's land use plan and development regulations include height limitations associated with the 35-foot building restriction line and the 7:1 height requirements. In addition, the proposed uses comply with the Zone B occupancy and use restrictions. Security fencing will be provided adjacent to the runway. In addition, no standing water exceeding the airport's criteria will be allowed as part of the project's drainage plan.

As you can read from the above, the Veterans Industrial Park 215 project has been designed in accordance with MARB concerns and regulations. As requested, please see below responses to your specific questions:

1) Security:

The VIP 215 Specific Plan, item 4.2.5, page 4-5, Security Elements addresses Cameras, Fencing, and Gating, specifically stating the following:

- ***Cameras: No cameras may be oriented towards the runway and cameras must not record base airfield operations.***
- ***Fencing: Along the runway/airport boundary on the Specific Plan area's eastern edge, special security fencing shall be used. Fencing must be minimum eight feet in height with three strands of barded wire. This fencing shall be of a durable material (may be chain link) subject to JPA and March Air Reserve Base review.***
- ***Gating: Pedestrian and vehicular access gates visible from public areas (i.e., parking lots, streets, sidewalks, etc.) shall be constructed of a durable material, such as tubular steel.***

a) Cameras and Walls:

- i) Addressed – see above.
- ii) Perimeter block wall: On December 2, 2015, MJPA, Hillwood, and MARB Security Forces meet to discuss the security needs of the VIP 215 project. MARB Security Forces requested a chain link fence with barbed wire (in lieu of block wall) be installed on the easterly perimeter to allow for ease of viewing activities of the tenants surrounding the Base. As well, according to March JPA, on July 21, 2016, MARB security forces reiterated that block walls impede their line of site, and request a fence instead; including no cameras facing towards the runway. Addressed – see above.

- b) The VIP 215 Project will not impede on MARB's mission to carry out Anti-Terrorism/Force Protection procedures or maneuvers.
- c) Hillwood understands the importance of MARB security forces to respond immediately to possible security risks and understands the right of the U.S. Air Force to inspect the premises of the VIP 215 project area. We will work with our legal counsel to craft the appropriate commercially-reasonable language for lease agreements. The language will allow for MARB and/or U.S. Air Force Security Forces to access the property during an imminent threat (same access as provided to law enforcement and emergency response teams), while providing reasonable notice to the tenant(s) absent an imminent threat.

2) Drainage

As stated in the VIP 215 Specific Plan, item 2.3.3, page 2-12, Storm Water Management – Project Drainage:

The site was divided into two parcels/drainage areas with stand-alone drainage facilities, consistent with the two Specific Plan Planning Areas. As noted above, the existing earthen channels will be realigned to the property's eastern edge, and a concrete trapezoidal channel constructed. On-site storm water will be collected, either by surface flow or storm drains, and directed to two bio-retention/detention basins. Each basin is sized to have storage capacity above the water quality treatment volume to detain higher storm events. A pump system will be used to convey water from the basins to the proposed channel. These pumps will regulate the rate of discharge to mimic pre-development flow rates. All drainage facilities are sized for the 100-year storm event. All observable water in both basins will be pumped out within 24 hours of the end of a storm event. This is a betterment from MJPA's standard criteria of 48 hours (and existing conditions). Figure 2-8, Drainage Plan, illustrates the drainage concept for the project.

Further, as stipulated in the VIP 215 Specific Plan, under General Plan Consistency Table, Land Use Goal 17 states:

The project will comply with the March JPA's NPDES New Development and Redevelopment Guidelines for projects under the March Joint Powers Authority and the Santa Ana Regional Water Quality Control Board Order No. R8-2002-0011. A hydrology study and water quality management plan (WQMP) will be prepared for the project. It is not presumed that project implementation would have a substantial impact on water quality standards or waste discharge requirements. The project will provide for a drainage plan to convey on-site flows to the existing drainage areas downstream of the project. A regional drainage solution is in process to convey off-site flows around the project site to the existing drainage facilities downstream. Any significant impacts to hydrology and water quality will be mitigated through the environmental review process and prior to development.

- a) Addressed – see above.
- b) Addressed – see above.

- c) MIPA and MARB have had multiple meetings, emails, and conference calls regarding the Project's proposed drainage solution. Most recently, on June 6th, 2018, MARB requested information on volume and flow rates related to ponding and drainage flows on the Airfield. A detailed analysis of offsite drainage was prepared by Albert A Webb Associates engineers. The detailed Technical Memorandum dated July 13, 2018 found the downstream floodplain limits to be less due to the improvements contemplated by the VIP 215 project. Specifically, the Historical flow to MARB is 2,327 cfs; the existing condition is 1,887 cfs; and the proposed with VIP 215 project is 1,767 cfs.

3) Glare

As stated in the VIP 215 Specific Plan, item 4.2.2 page 4-4, Materials and Colors:

- *Materials shall be of a non-reflective material, including exterior ductwork, windows, and roofs.*
- *Highly reflective materials on elevations facing the runway or the aircraft approach path are prohibited.*

As stated in the VIP 215 Specific Plan, item 4.2.3 page 4-4, Windows and Doors:

- *Appropriate treatments consist of functional glass use, a balance of glazing and wall surfaces, with no highly reflective surfaces facing the runway.*
- *Mirrored or highly reflective glass is no permitted.*

As stated in the VIP 215 Specific Plan, item 4.3.5, page 4-13, Exterior Lighting:

- *Lighting will utilize high efficiency technologies, dark-sky cutoffs, strategic orientation to avoid spillover into adjacent properties, the adjacent runway, and open space areas, and appropriate shielding or recesses to minimize glare and reflections.*
- *Exterior lighting should be unobtrusive and not cause glare or spillover into neighboring properties, especially within 100 feet of open space or adjacent runways.*

- a) Addressed – see above.
b) Development standards designed per VIP 215 Specific Plan item 1.2 page 1-1, the Air Installation Compatible Use Zone Study for March Air Reserve Base (2108) impacts the VIP 215 Specific Plan; and item 3.1.9 page 3-3 March Air Reserve Base Performance Standards.
c) Addressed in VIP 215 Specific Plan item 3.2.4 h) Prohibited Uses – Solar Panels.
d) Addressed – see above.

4) Safety

As stated in the VIP 215 Specific Plan, item 3.2.4. Prohibited Uses:

- d) Above ground Petroleum Storage containers and below ground storage containers in excess of 10,000 gallons.*

e) Uses inconsistent with the B2 Airport Land Use Compatibility Zone as identified in the current Airport Land Use Compatibility Plan.

a) Compliant with Zone B2 – see above. VIP 215 project is not located in a Clear Zone.

5) Aviation Land Use Capability and Designation.

***As stated in the VIP 215 Specific Plan, item 1.4 Discretionary Items:
The Specific Plan Area is presently un-zoned. The establishment of the Specific Plan for the proposed project will provide for an SP-8 designation on the March JPA Zoning Map with an underlying Aviation Designation.***

As stated in the VIP 215 Specific Plan, item 3.2.1 Permitted Uses, Light Manufacturing and Assembly including aviation-related manufacturing is a permitted use. Aviation Related Facilities are listed as a Conditional Use subject to further Environmental Review.

a) Addressed – see above. It is important to note that one of the key Project Objectives listed in the VIP 215 Specific Plan, item 2.1.1, is to “Facilitate the development of underutilized land currently planned for aviation-related uses that maximize the use of the site and responds to market demand within the VIP 215 Specific Plan area and surrounding region for a large format logistics center.” The VIP 215 Project will not obstruct future aviation use or the right to obtain taxiway access to the runway from the project property

Thank you for your time and attention to the success of the Veterans Industrial Park 215 project. Should you have any questions, please do not hesitate to contact me directly at (909) 382-0033.

Respectfully,
HILLWOOD



John M. Magness
Senior Vice President

c.c. Jeff Smith, March Joint Powers Authority
Matt Evans, March Joint Powers Authority
Gary Gosliga, March Inland Port Airport Authority
Paul Rull, Riverside County Airport Land Use Commission



DEPARTMENT OF THE AIR FORCE
AIR FORCE RESERVE COMMAND



1 June 2018

MEMORANDUM FOR LOCAL GOVERNMENTS IN THE VICINITY OF MARCH ARB

FROM: 452 AMW/CC
2145 Graeber Street, Suite 117
March ARB CA 92518-1667

SUBJECT: Position on Joint Land Use Study by RCALUC

1. March Air Reserve Base (MARB) personnel are supportive of the Riverside County Airport Land Use Commission (RCALUC) pursuing and undertaking a Joint Land Use Study (JLUS).
2. A JLUS is a tool used to analyze impacts by operations of a military installation on local jurisdictions as well as land use impacts to operations on a military installation. It is a cooperative study to help provide a policy framework to support adoption and implementation of compatible development regulations near a military base.
3. Certain land uses and conditions, as well as operations by the United States Air Force (USAF) and Air Force Reserve Command (AFRC) in the vicinity of MARB, should be studied to help safeguard the military mission and protect the health, safety and welfare of the public.
4. Areas of study that are of interest to the USAF and AFRC mission include, but are not limited to: 1.) population densities in the vicinity of MARB; 2.) rising ground water conditions; 3.) storm water and flooding potential caused by development around the installation; 4.) study of clear zones/accident potential zones for Runway 12-30; 5.) landscaping potential to attract wildlife causing flight safety impacts; 6.) increased glare from passive reflective roof surfaces and solar panel/photovoltaic development; and 7.) mitigating land use and traffic impacts within clear zones.
5. The MARB Final Air Installation Compatibility Use Zone Study (AICUZ) was released to the public on February 21, 2018 to all surrounding jurisdictions, to include the Cities of Riverside, Moreno Valley, Perris; County of Riverside; and March Joint Powers Authority. The last time this document was updated was in 2005, which was used in part as the basis of the Riverside County's Airport Land Use Commission's March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan (ALUCP). Any JLUS effort going forward should use the AICUZ as a launching-off point for continued discussion and collaboration, and updating of the ALUCP.

6. Updating the ALUCP without conducting a JLUS could potentially cause key impacts to not be analyzed and studied. It is MARB personnel's desire to have as much cooperation and collaboration with local land use authorities to ensure the health, safety and welfare of the public and USAF members alike, as well as help sustain the military mission.

7. Based on the above-stated conditions we recommend that a JLUS study be conducted prior to the adoption of an updated ALUCP.

BURGER.MATTH Digitally signed by
EW.J.1139535797 BURGER.MATTHEW.J.1139535
797
Date: 2018.06.13 09:48:06 -07'00'

MATTHEW J BURGER, Col, USAF
Commander

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2016-AWP-12028-OE

Issued Date: 01/26/2017

JOHN MAGNESS
 RIVERSIDE INLAND DEVELOPMENT, LLC
 901 VIA PIEMONTE
 SUITE 175
 ONTARIO, CA 91764

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Building Veterans Industrial Park
Location:	Perris, CA
Latitude:	33-52-40.79N NAD 83
Longitude:	117-15-49.14W
Heights:	1516 feet site elevation (SE)
	48 feet above ground level (AGL)
	1564 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 1.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 07/26/2018 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates , heights, frequency(ies) and power . Any changes in coordinates , heights, and frequencies or use of greater power will void this determination. Any future construction or alteration , including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (310) 725-6557. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2016-AWP-12028-OE.

Signature Control No: 311816032-320352868

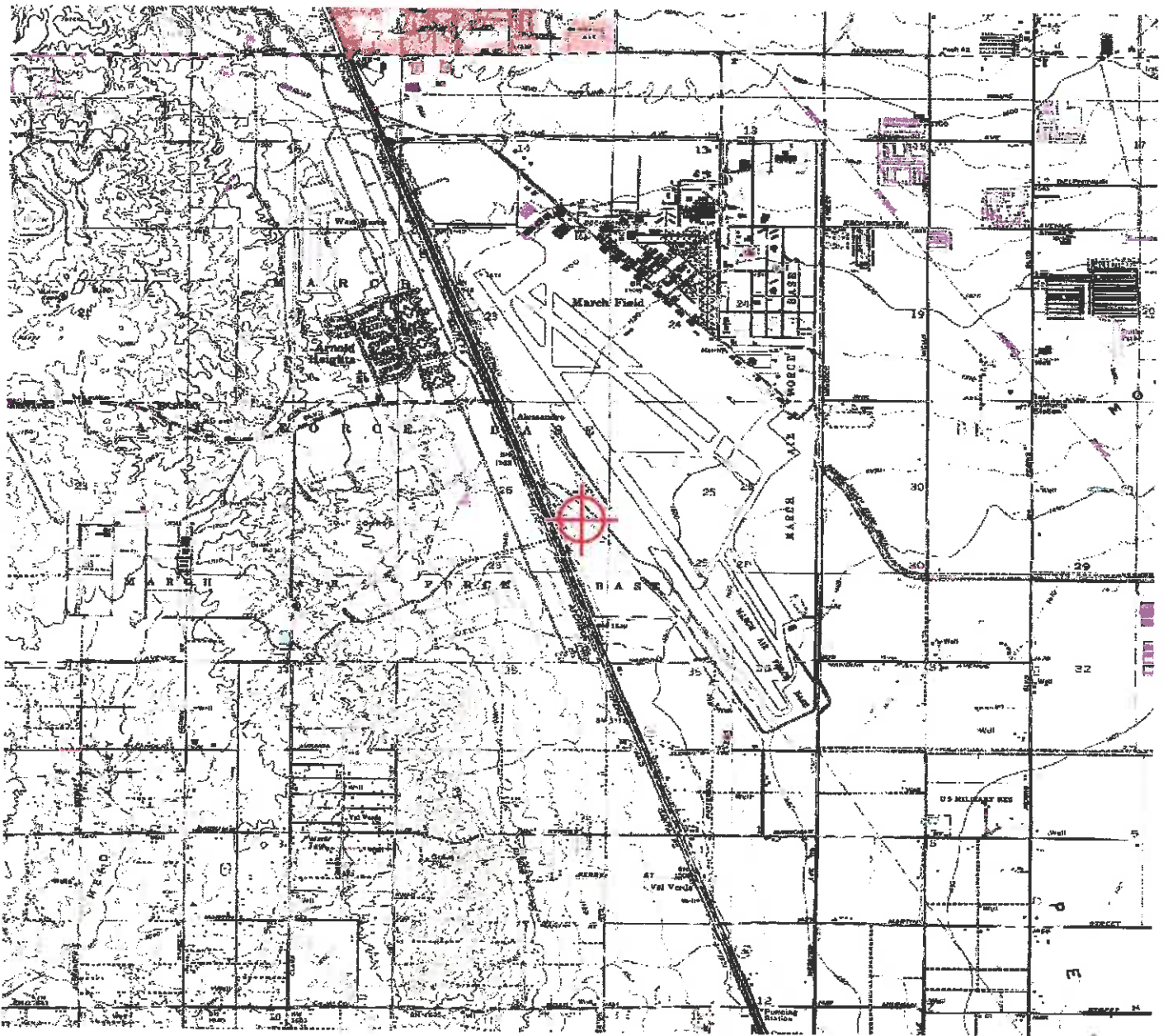
(DNE)

Karen McDonald
Specialist

Attachment(s)

Map(s)

TOPO Map for ASN 2016-AWP-12028-OE





Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2016-AWP-12028-OE

Issued Date: 08/21/2018

JOHN MAGNESS
RIVERSIDE INLAND DEVELOPMENT, LLC
901 VIA PIEMONTE
SUITE 175
ONTARIO, CA 91764

**** Extension ****

A Determination was issued by the Federal Aviation Administration (FAA) concerning:

Structure:	Building Veterans Industrial Park
Location:	Perris, CA
Latitude:	33-52-40.79N NAD 83
Longitude:	117-15-49.14W
Heights:	1516 feet site elevation (SE) 48 feet above ground level (AGL) 1564 feet above mean sea level (AMSL)

In response to your request for an extension of the effective period of the determination, the FAA has reviewed the aeronautical study in light of current aeronautical operations in the area of the structure and finds that no significant aeronautical changes have occurred which would alter the determination issued for this structure.

Accordingly, pursuant to the authority delegated to me, the effective period of the determination issued under the above cited aeronautical study number is hereby extended and will expire on 02/21/2020 unless otherwise extended, revised, or terminated by this office. You must adhere to all conditions identified in the original determination.

This extension issued in accordance with 49 U.S.C., Section 44718 and, if applicable, Title 14 of the Code of Federal Regulations, part 77, concerns the effect of the structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (310) 725-6557, or karen.mcdonald@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2016-AWP-12028-OE.

Signature Control No: 311816032-382228437
Karen McDonald
Specialist

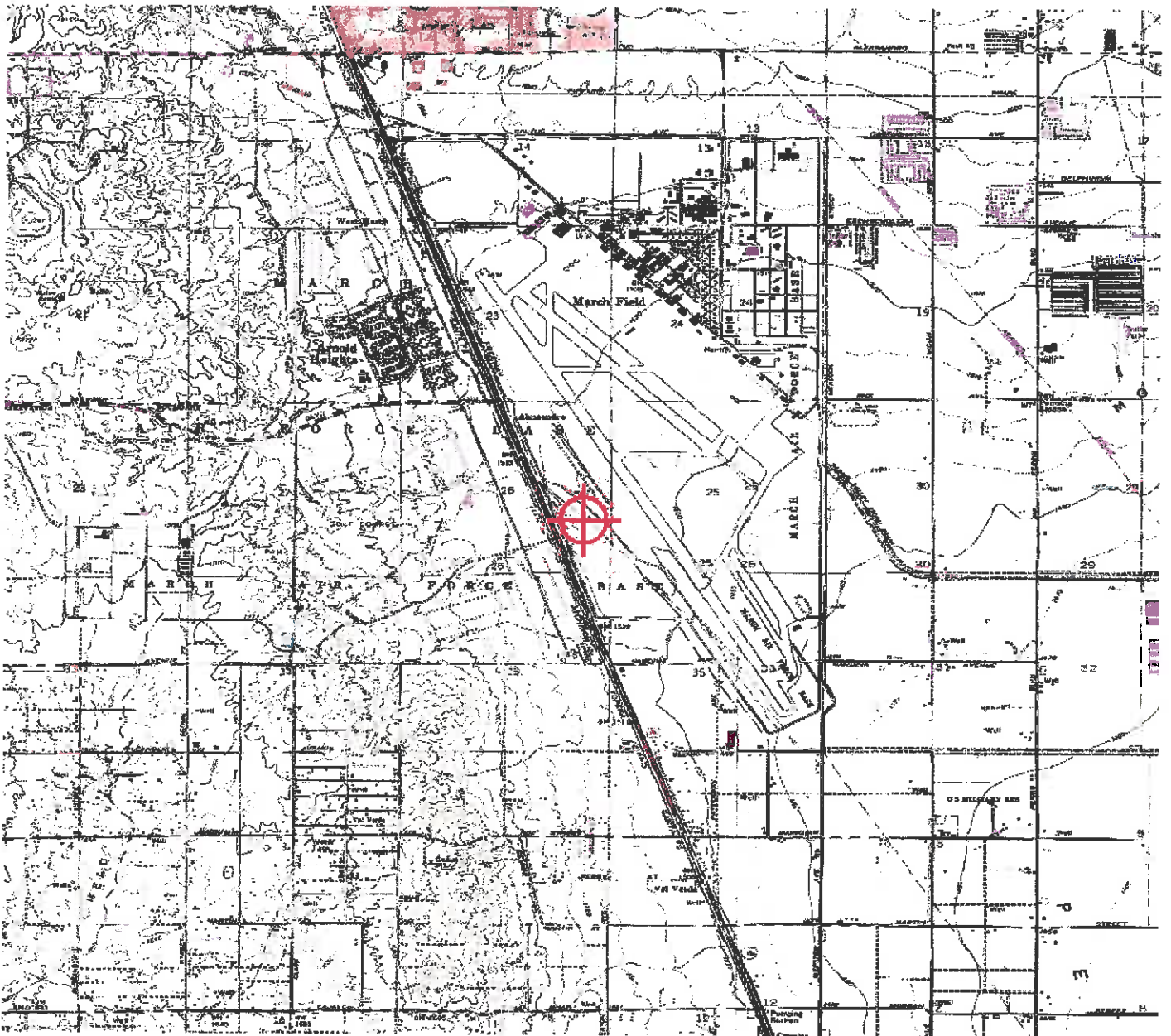
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Attachment(s)
Additional Information
Map(s)

Additional information for ASN 2016-AWP-12028-OE

ALL REQUIREMENTS AND CONDITIONS ON ORIGINAL AIRSPACE DETERMINATION REMAIN IN EFFECT.

TOPO Map for ASN 2016-AWP-12028-OE





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Issued Date: 01/26/2017

JOHN MAGNESS
 RIVERSIDE INLAND DEVELOPMENT, LLC
 901 VIA PIEMONTE
 SUITE 175
 ONTARIO, CA 91764

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building VIP - Bldg 1 (NW Corner)
 Location: Perris, CA
 Latitude: 33-52-43.67N NAD 83
 Longitude: 117-15-56.64W
 Heights: 1523 feet site elevation (SE)
 48 feet above ground level (AGL)
 1571 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 1.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 07/26/2018 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
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- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

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If we can be of further assistance, please contact our office at (310) 725-6557. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2016-AWP-12029-OE.

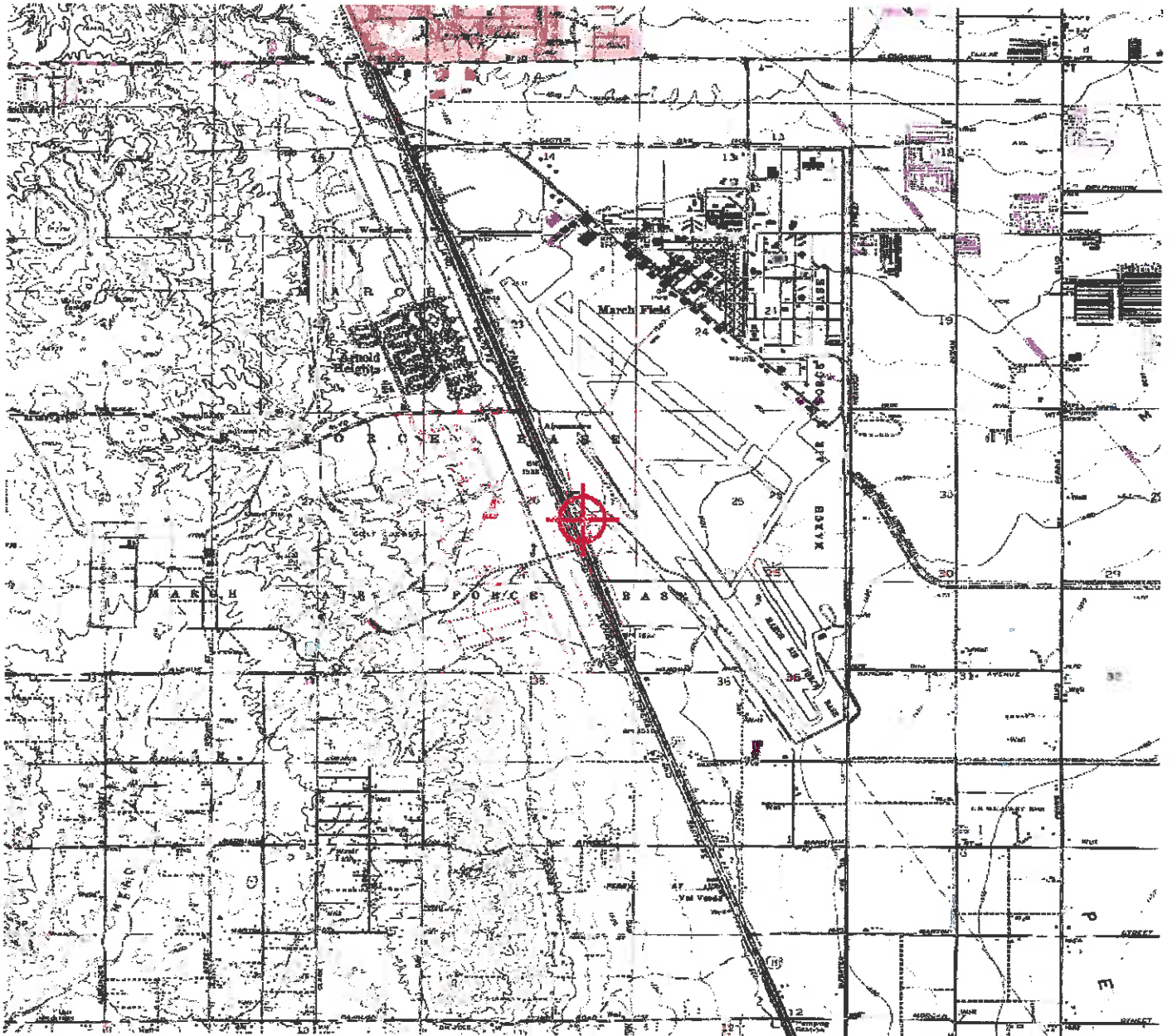
Signature Control No: 311816033-320352867

(DNE)

Karen McDonald
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2016-AWP-12029-OE





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Issued Date: 08/21/2018

JOHN MAGNESS
RIVERSIDE INLAND DEVELOPMENT, LLC
901 VIA PIEMONTE
SUITE 175
ONTARIO, CA 91764

**** Extension ****

A Determination was issued by the Federal Aviation Administration (FAA) concerning:

Structure:	Building VIP - Bldg 1 (NW Corner)
Location:	Perris, CA
Latitude:	33-52-43.67N NAD 83
Longitude:	117-15-56.64W
Heights:	1523 feet site elevation (SE) 48 feet above ground level (AGL) 1571 feet above mean sea level (AMSL)

In response to your request for an extension of the effective period of the determination, the FAA has reviewed the aeronautical study in light of current aeronautical operations in the area of the structure and finds that no significant aeronautical changes have occurred which would alter the determination issued for this structure.

Accordingly, pursuant to the authority delegated to me, the effective period of the determination issued under the above cited aeronautical study number is hereby extended and will expire on 02/21/2020 unless otherwise extended, revised, or terminated by this office. You must adhere to all conditions identified in the original determination.

This extension issued in accordance with 49 U.S.C., Section 44718 and, if applicable, Title 14 of the Code of Federal Regulations, part 77, concerns the effect of the structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (310) 725-6557, or karen.mcdonald@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2016-AWP-12029-OE.

Signature Control No: 311816033-382228444
Karen McDonald
Specialist

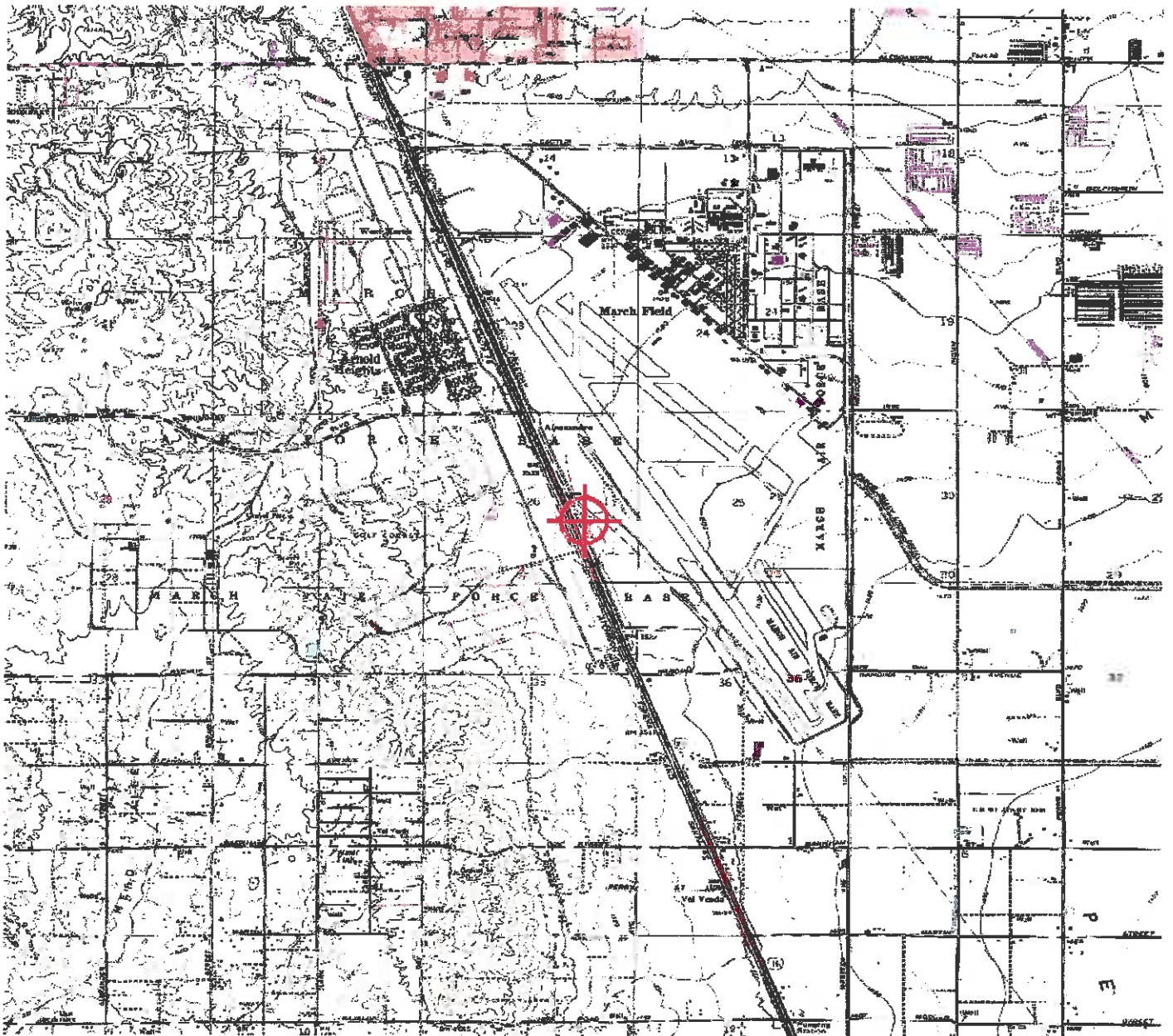
(EXT)

Attachment(s)
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Map(s)

Additional information for ASN 2016-AWP-12029-OE

ALL REQUIREMENTS AND CONDITIONS ON ORIGINAL AIRSPACE DETERMINATION REMAIN IN EFFECT.

TOPO Map for ASN 2016-AWP-12029-OE





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 Fort Worth, TX 76177

Aeronautical Study No.
 2016-AWP-12030-OE

Issued Date: 01/26/2017

JOHN MAGNESS
 RIVERSIDE INLAND DEVELOPMENT, LLC
 901 VIA PIEMONTE
 SUITE 175
 ONTARIO, CA 91764

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building VIP - Bldg 1 (NE Corner)
 Location: Perris, CA
 Latitude: 33-52-47.08N NAD 83
 Longitude: 117-15-50.59W
 Heights: 1523 feet site elevation (SE)
 48 feet above ground level (AGL)
 1571 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does exceed obstruction standards but would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

As a condition to this Determination, the structure is to be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 1, Obstruction Marking and Lighting, red lights - Chapters 4,5(Red),&12.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 07/26/2018 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
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If we can be of further assistance, please contact our office at (310) 725-6557. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2016-AWP-12030-OE.

Signature Control No: 311816034-320353133
Karen McDonald
Specialist

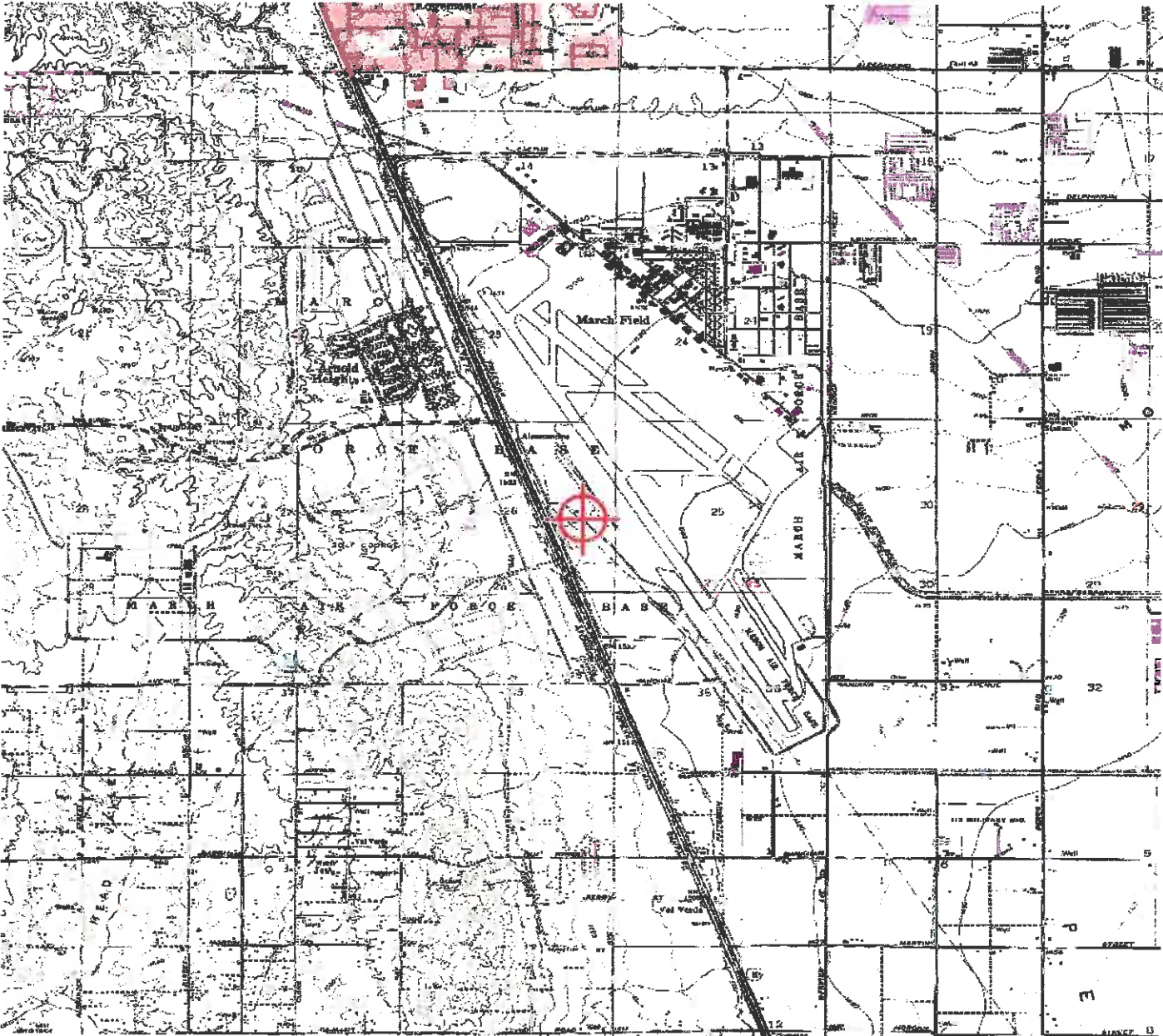
(EBO)

Attachment(s)
Additional Information
Map(s)

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RECOMMEND RED OBSTRUCTION LIGHTING INSTALLED ON THIS POINT OF BUILDING.
HEIGHT PENETRATES AN FAR PART 77 PROTECTED SURFACE; 77.21(b)(4) *RIV[EXISTING]
[NASR] - RWY 14/32[EXISTING][NASR]: Transitional Surface ---> Exceeds by 4 ft.

TOPO Map for ASN 2016-AWP-12030-OE





Mail Processing Center
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Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2016-AWP-12030-OE

Issued Date: 08/21/2018

JOHN MAGNESS
RIVERSIDE INLAND DEVELOPMENT, LLC
901 VIA PIEMONTE
SUITE 175
ONTARIO, CA 91764

**** Extension ****

A Determination was issued by the Federal Aviation Administration (FAA) concerning:

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Signature Control No: 311816034-382228441
Karen McDonald
Specialist

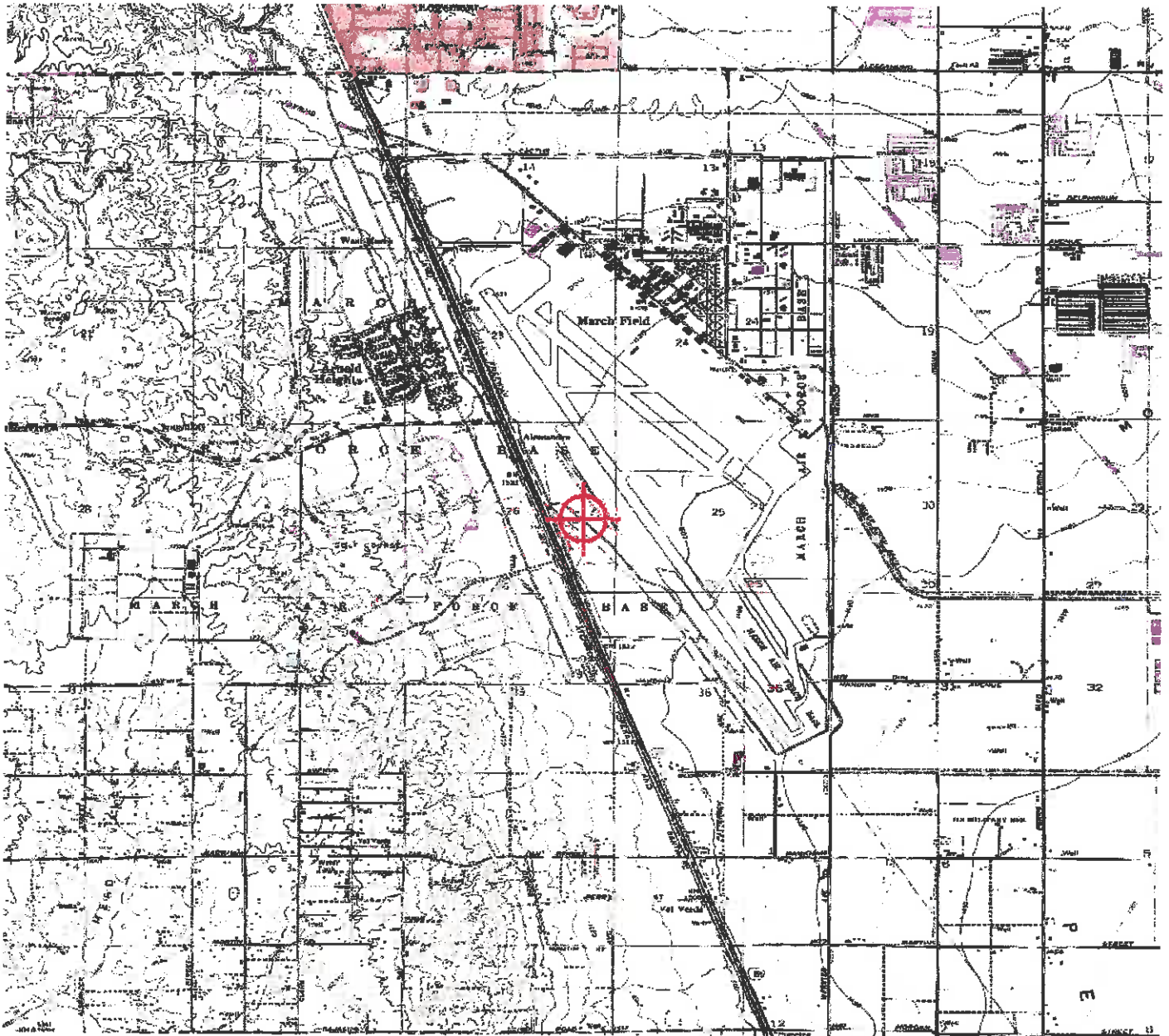
(EXT)

Attachment(s)
Additional Information
Map(s)

Additional information for ASN 2016-AWP-12030-OE

ALL REQUIREMENTS AND CONDITIONS ON ORIGINAL AIRSPACE DETERMINATION REMAIN IN EFFECT.

TOPO Map for ASN 2016-AWP-12030-OE





Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2016-AWP-12031-OE

Issued Date: 01/26/2017

JOHN MAGNESS
RIVERSIDE INLAND DEVELOPMENT, LLC
901 VIA PIEMONTE
SUITE 175
ONTARIO, CA 91764

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building VIP - Bldg 1 (SW Corner)
Location: Perris, CA
Latitude: 33-52-29.91N NAD 83
Longitude: 117-15-47.67W
Heights: 1515 feet site elevation (SE)
48 feet above ground level (AGL)
1563 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 1.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 07/26/2018 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates , heights, frequency(ies) and power . Any changes in coordinates , heights, and frequencies or use of greater power will void this determination. Any future construction or alteration , including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (310) 725-6557. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2016-AWP-12031-OE.

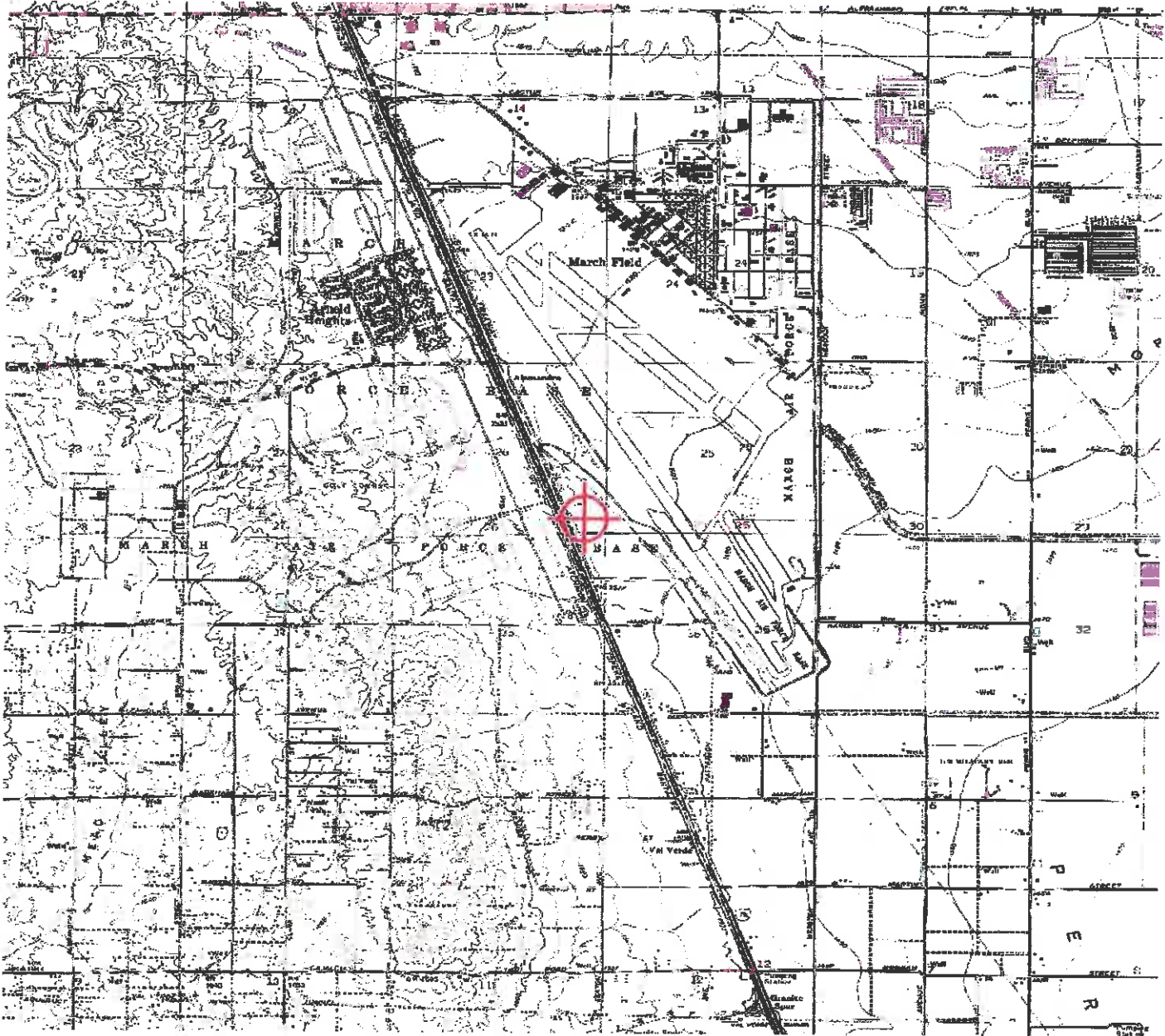
Signature Control No: 311816035-320352866

(DNE)

Karen McDonald
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2016-AWP-12031-OE





Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2016-AWP-12031-OE

Issued Date: 08/21/2018

JOHN MAGNESS
RIVERSIDE INLAND DEVELOPMENT, LLC
901 VIA PIEMONTE
SUITE 175
ONTARIO, CA 91764

**** Extension ****

A Determination was issued by the Federal Aviation Administration (FAA) concerning:

Structure:	Building VIP - Bldg 1 (SW Corner)
Location:	Perris, CA
Latitude:	33-52-29.91N NAD 83
Longitude:	117-15-47.67W
Heights:	1515 feet site elevation (SE) 48 feet above ground level (AGL) 1563 feet above mean sea level (AMSL)

In response to your request for an extension of the effective period of the determination, the FAA has reviewed the aeronautical study in light of current aeronautical operations in the area of the structure and finds that no significant aeronautical changes have occurred which would alter the determination issued for this structure.

Accordingly, pursuant to the authority delegated to me, the effective period of the determination issued under the above cited aeronautical study number is hereby extended and will expire on 02/21/2020 unless otherwise extended, revised, or terminated by this office. You must adhere to all conditions identified in the original determination.

This extension issued in accordance with 49 U.S.C., Section 44718 and, if applicable, Title 14 of the Code of Federal Regulations, part 77, concerns the effect of the structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (310) 725-6557, or karen.mcdonald@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2016-AWP-12031-OE.

Signature Control No: 311816035-382228438
Karen McDonald
Specialist

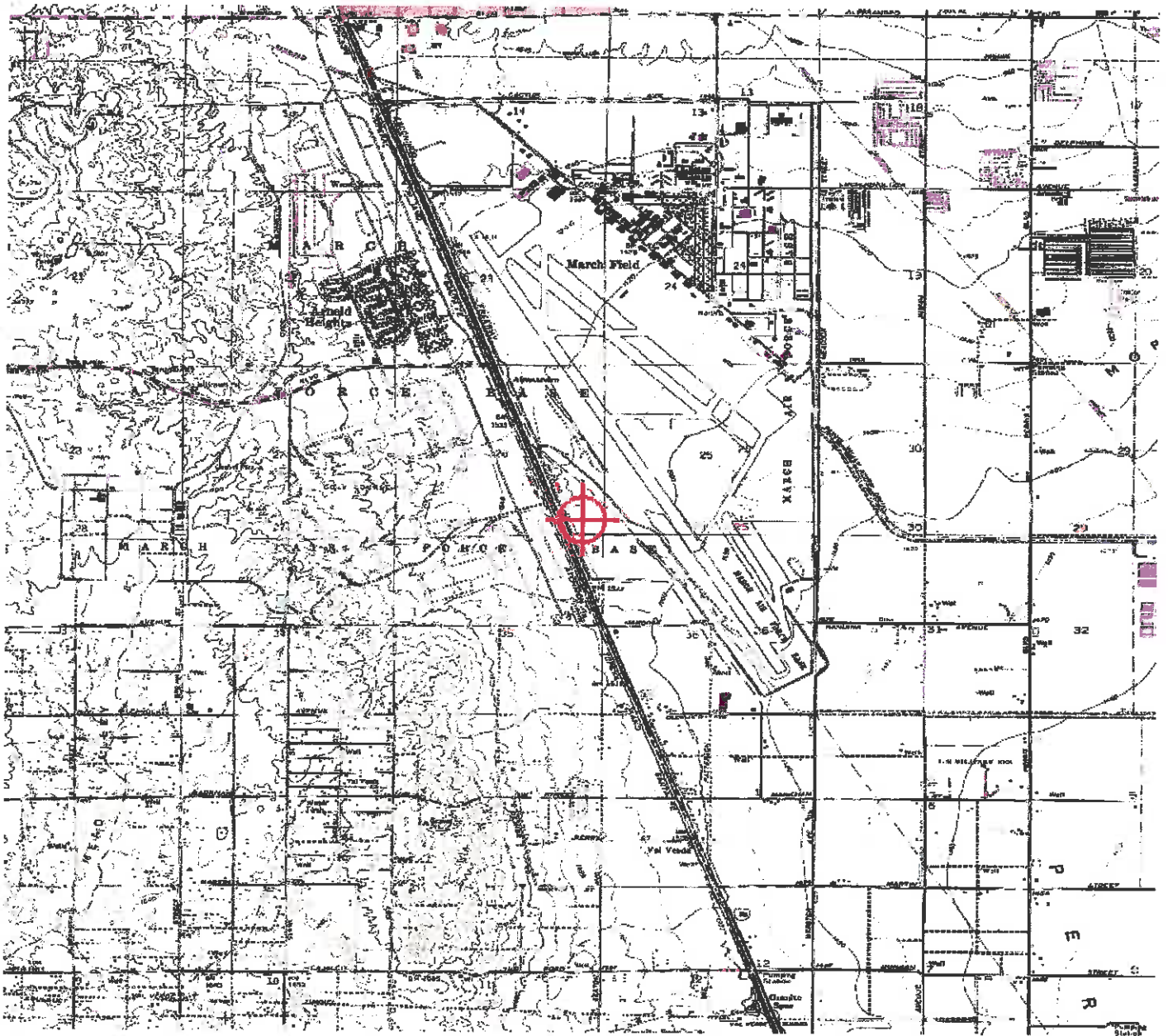
(EXT)

Attachment(s)
Additional Information
Map(s)

Additional information for ASN 2016-AWP-12031-OE

ALL REQUIREMENTS AND CONDITIONS ON ORIGINAL AIRSPACE DETERMINATION REMAIN IN EFFECT.

TOPO Map for ASN 2016-AWP-12031-OE



Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2016-AWP-12032-OE

Issued Date: 01/26/2017

JOHN MAGNESS
RIVERSIDE INLAND DEVELOPMENT, LLC
901 VIA PIEMONTE
SUITE 175
ONTARIO, CA 91764

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Building VIP - Bldg 1 (SE Corner)
Location:	Perris, CA
Latitude:	33-52-33.26N NAD 83
Longitude:	117-15-40.76W
Heights:	1515 feet site elevation (SE) 48 feet above ground level (AGL) 1563 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does exceed obstruction standards but would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

As a condition to this Determination, the structure is to be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 1, Obstruction Marking and Lighting, red lights - Chapters 4,5(Red),&12.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

At least 10 days prior to start of construction (7460-2, Part 1)
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 07/26/2018 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates , heights, frequency(ies) and power . Any changes in coordinates , heights, and frequencies or use of greater power will void this determination. Any future construction or alteration , including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (310) 725-6557. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2016-AWP-12032-OE.

Signature Control No: 311816036-320353961

(EBO)

Karen McDonald
Specialist

Attachment(s)
Additional Information
Map(s)

Additional information for ASN 2016-AWP-12032-OE

RECOMMEND RED OBSTRUCTION LIGHTING INSTALLED ON THIS POINT OF BUILDING.
HEIGHT PENETRATES AN FAR PART 77 PROTECTED SURFACE; 77.21(b)(4) *RIV[EXISTING]
[NASR] - RWY 14/32[EXISTING][NASR]: Transitional Surface ---> Exceeds by 2 ft.

TOPO Map for ASN 2016-AWP-12032-OE



Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2016-AWP-12032-OE

Issued Date: 08/21/2018

JOHN MAGNESS
RIVERSIDE INLAND DEVELOPMENT, LLC
901 VIA PIEMONTE
SUITE 175
ONTARIO, CA 91764

**** Extension ****

A Determination was issued by the Federal Aviation Administration (FAA) concerning:

Structure:	Building VIP - Bldg 1 (SE Corner)
Location:	Perris, CA
Latitude:	33-52-33.26N NAD 83
Longitude:	117-15-40.76W
Heights:	1515 feet site elevation (SE) 48 feet above ground level (AGL) 1563 feet above mean sea level (AMSL)

In response to your request for an extension of the effective period of the determination, the FAA has reviewed the aeronautical study in light of current aeronautical operations in the area of the structure and finds that no significant aeronautical changes have occurred which would alter the determination issued for this structure.

Accordingly, pursuant to the authority delegated to me, the effective period of the determination issued under the above cited aeronautical study number is hereby extended and will expire on 02/21/2020 unless otherwise extended, revised, or terminated by this office. You must adhere to all conditions identified in the original determination.

This extension issued in accordance with 49 U.S.C., Section 44718 and, if applicable, Title 14 of the Code of Federal Regulations, part 77, concerns the effect of the structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (310) 725-6557, or karen.mcdonald@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2016-AWP-12032-OE.

Signature Control No: 311816036-382228440

Karen McDonald
Specialist

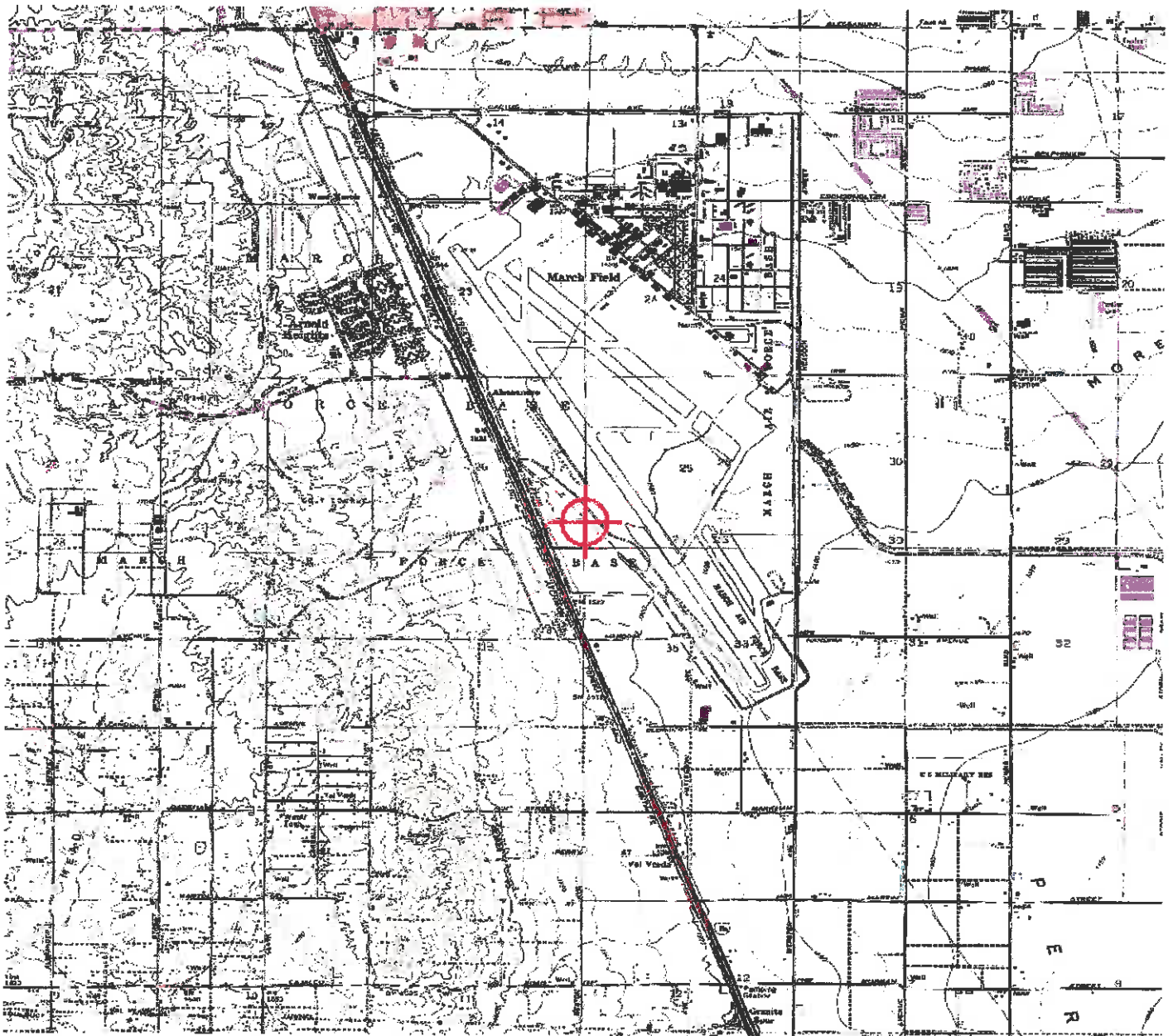
(EXT)

Attachment(s)
Additional Information
Map(s)

Additional information for ASN 2016-AWP-12032-OE

ALL REQUIREMENTS AND CONDITIONS ON ORIGINAL AIRSPACE DETERMINATION REMAIN IN EFFECT.

TOPO Map for ASN 2016-AWP-12032-OE





Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2016-AWP-12033-OE

Issued Date: 01/26/2017

JOHN MAGNESS
 RIVERSIDE INLAND DEVELOPMENT, LLC
 901 VIA PIEMONTE
 SUITE 175
 ONTARIO, CA 91764

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building VIP - Bldg 2 (NW Corner)
 Location: Perris, CA
 Latitude: 33-52-27.33N NAD 83
 Longitude: 117-15-46.66W
 Heights: 1516 feet site elevation (SE)
 48 feet above ground level (AGL)
 1564 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 1.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 07/26/2018 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates , heights, frequency(ies) and power . Any changes in coordinates , heights, and frequencies or use of greater power will void this determination. Any future construction or alteration , including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (310) 725-6557. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2016-AWP-12033-OE.

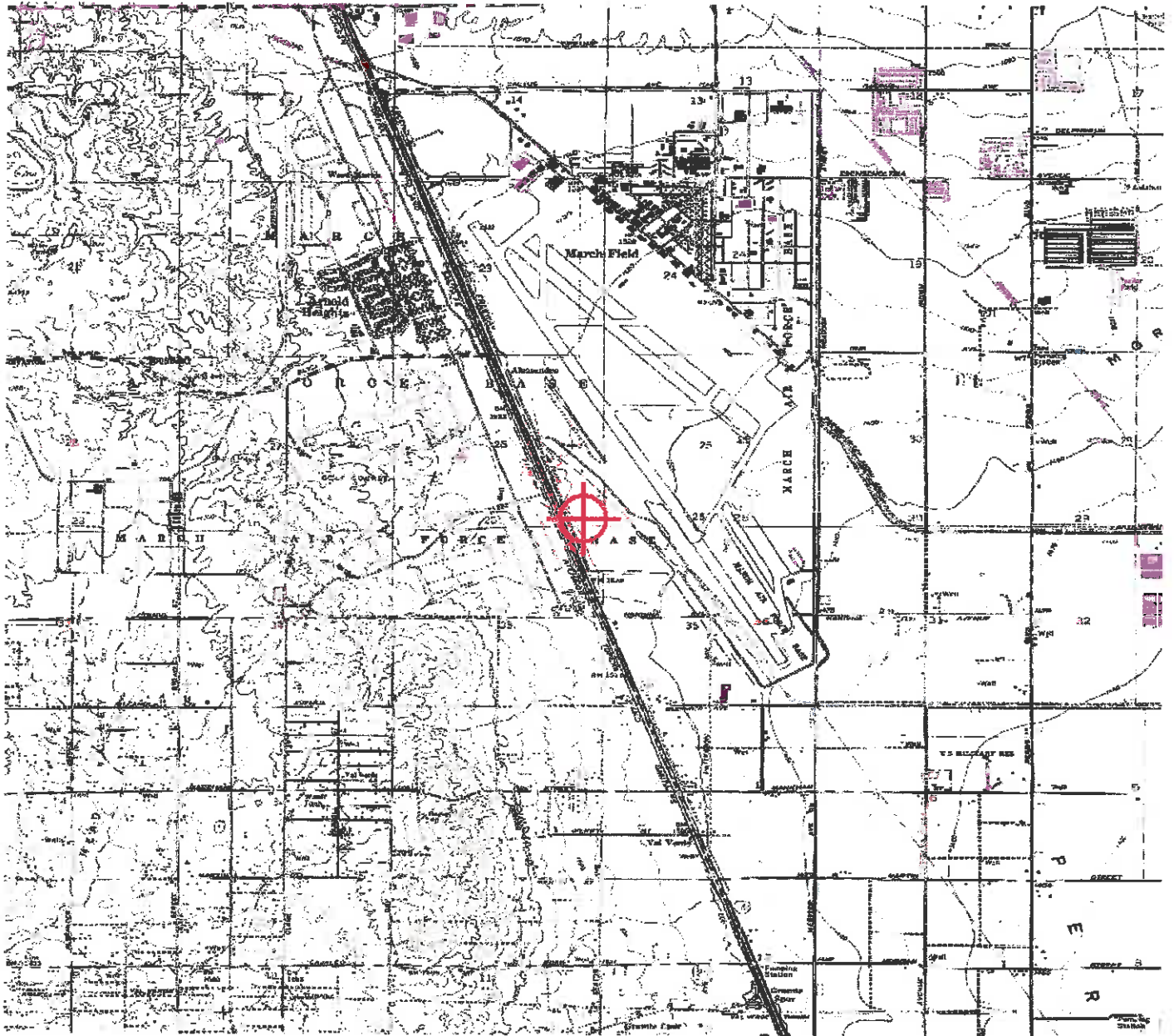
Signature Control No: 311816037-320352869

(DNE)

Karen McDonald
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2016-AWP-12033-OE





Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2016-AWP-12033-OE

Issued Date: 08/21/2018

JOHN MAGNESS
RIVERSIDE INLAND DEVELOPMENT, LLC
901 VIA PIEMONTE
SUITE 175
ONTARIO, CA 91764

**** Extension ****

A Determination was issued by the Federal Aviation Administration (FAA) concerning:

Structure:	Building VIP - Bldg 2 (NW Corner)
Location:	Perris, CA
Latitude:	33-52-27.33N NAD 83
Longitude:	117-15-46.66W
Heights:	1516 feet site elevation (SE) 48 feet above ground level (AGL) 1564 feet above mean sea level (AMSL)

In response to your request for an extension of the effective period of the determination, the FAA has reviewed the aeronautical study in light of current aeronautical operations in the area of the structure and finds that no significant aeronautical changes have occurred which would alter the determination issued for this structure.

Accordingly, pursuant to the authority delegated to me, the effective period of the determination issued under the above cited aeronautical study number is hereby extended and will expire on 02/21/2020 unless otherwise extended, revised, or terminated by this office. You must adhere to all conditions identified in the original determination.

This extension issued in accordance with 49 U.S.C., Section 44718 and, if applicable, Title 14 of the Code of Federal Regulations, part 77, concerns the effect of the structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (310) 725-6557, or karen.mcdonald@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2016-AWP-12033-OE.

Signature Control No: 311816037-382228439
Karen McDonald
Specialist

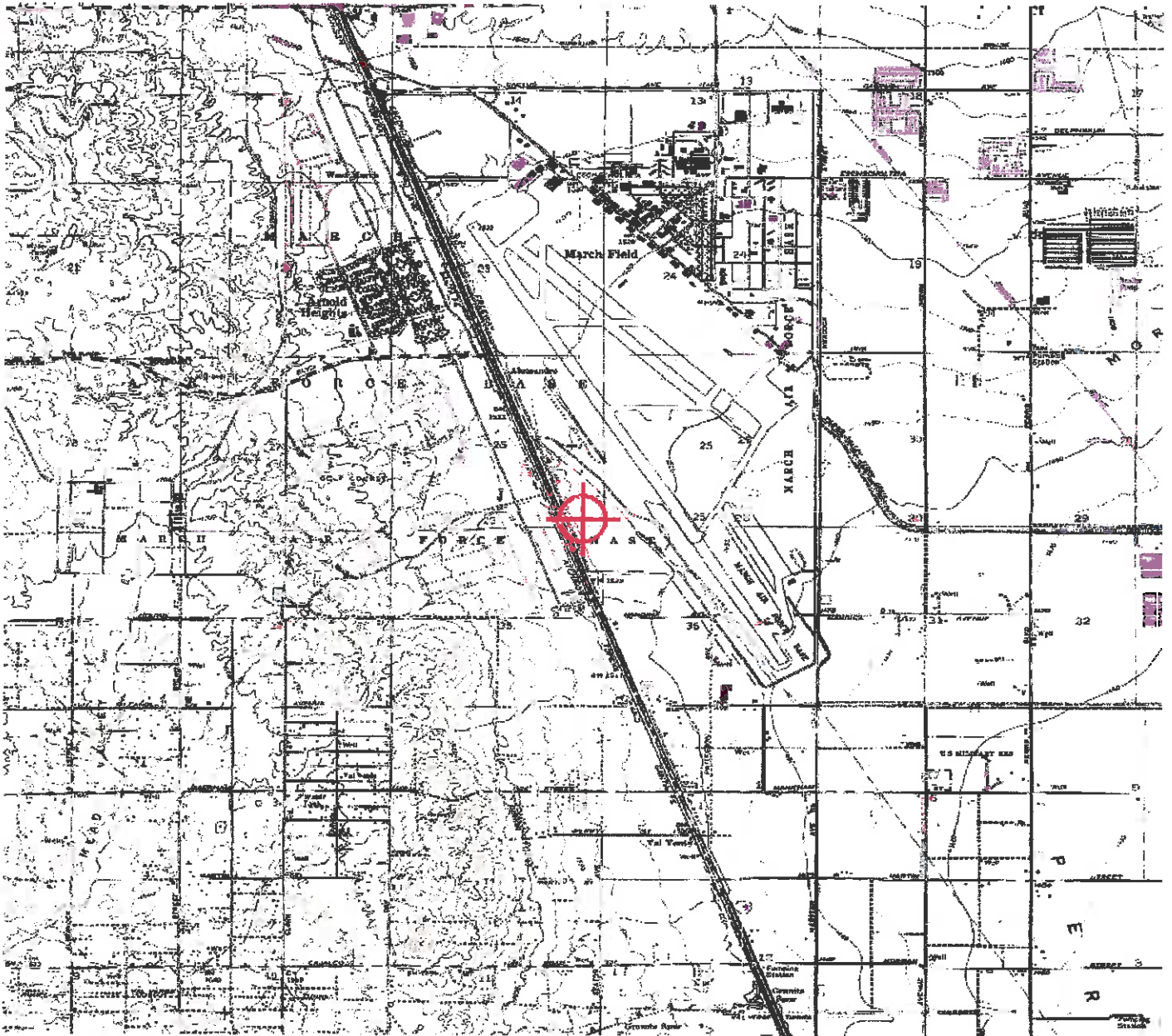
(EXT)

Attachment(s)
Additional Information
Map(s)

Additional information for ASN 2016-AWP-12033-OE

ALL REQUIREMENTS AND CONDITIONS ON ORIGINAL AIRSPACE DETERMINATION REMAIN IN EFFECT.

TOPO Map for ASN 2016-AWP-12033-OE



Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2016-AWP-12034-OE

Issued Date: 01/26/2017

JOHN MAGNESS
RIVERSIDE INLAND DEVELOPMENT, LLC
901 VIA PIEMONTE
SUITE 175
ONTARIO, CA 91764

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building VIP - Bldg 2 (NE Corner)
Location: Perris, CA
Latitude: 33-52-31.08N NAD 83
Longitude: 117-15-39.21W
Heights: 1516 feet site elevation (SE)
48 feet above ground level (AGL)
1564 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does exceed obstruction standards but would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

As a condition to this Determination, the structure is to be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 1, Obstruction Marking and Lighting, red lights - Chapters 4,5(Red),&12.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 07/26/2018 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates , heights, frequency(ies) and power . Any changes in coordinates , heights, and frequencies or use of greater power will void this determination. Any future construction or alteration , including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (310) 725-6557. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2016-AWP-12034-OE.

Signature Control No: 311816038-320354067

(EBO)

Karen McDonald
Specialist

Attachment(s)
Additional Information
Map(s)

Additional information for ASN 2016-AWP-12034-OE

RECOMMEND RED OBSTRUCTION LIGHTING INSTALLED ON THIS POINT OF BUILDING.
HEIGHT PENETRATES AN FAR PART 77 PROTECTED SURFACE; 77.21(b)(4) *RIV[EXISTING]
[NASR] - RWY 14/32[EXISTING][NASR]: Transitional Surface ---> Exceeds by 4 ft.



Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2016-AWP-12034-OE

Issued Date: 08/21/2018

JOHN MAGNESS
RIVERSIDE INLAND DEVELOPMENT, LLC
901 VIA PIEMONTE
SUITE 175
ONTARIO, CA 91764

**** Extension ****

A Determination was issued by the Federal Aviation Administration (FAA) concerning:

Structure:	Building VIP - Bldg 2 (NE Corner)
Location:	Perris, CA
Latitude:	33-52-31.08N NAD 83
Longitude:	117-15-39.21W
Heights:	1516 feet site elevation (SE) 48 feet above ground level (AGL) 1564 feet above mean sea level (AMSL)

In response to your request for an extension of the effective period of the determination, the FAA has reviewed the aeronautical study in light of current aeronautical operations in the area of the structure and finds that no significant aeronautical changes have occurred which would alter the determination issued for this structure.

Accordingly, pursuant to the authority delegated to me, the effective period of the determination issued under the above cited aeronautical study number is hereby extended and will expire on 02/21/2020 unless otherwise extended, revised, or terminated by this office. You must adhere to all conditions identified in the original determination.

This extension issued in accordance with 49 U.S.C., Section 44718 and, if applicable, Title 14 of the Code of Federal Regulations, part 77, concerns the effect of the structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (310) 725-6557, or karen.mcdonald@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2016-AWP-12034-OE.

Signature Control No: 311816038-382228442
Karen McDonald
Specialist

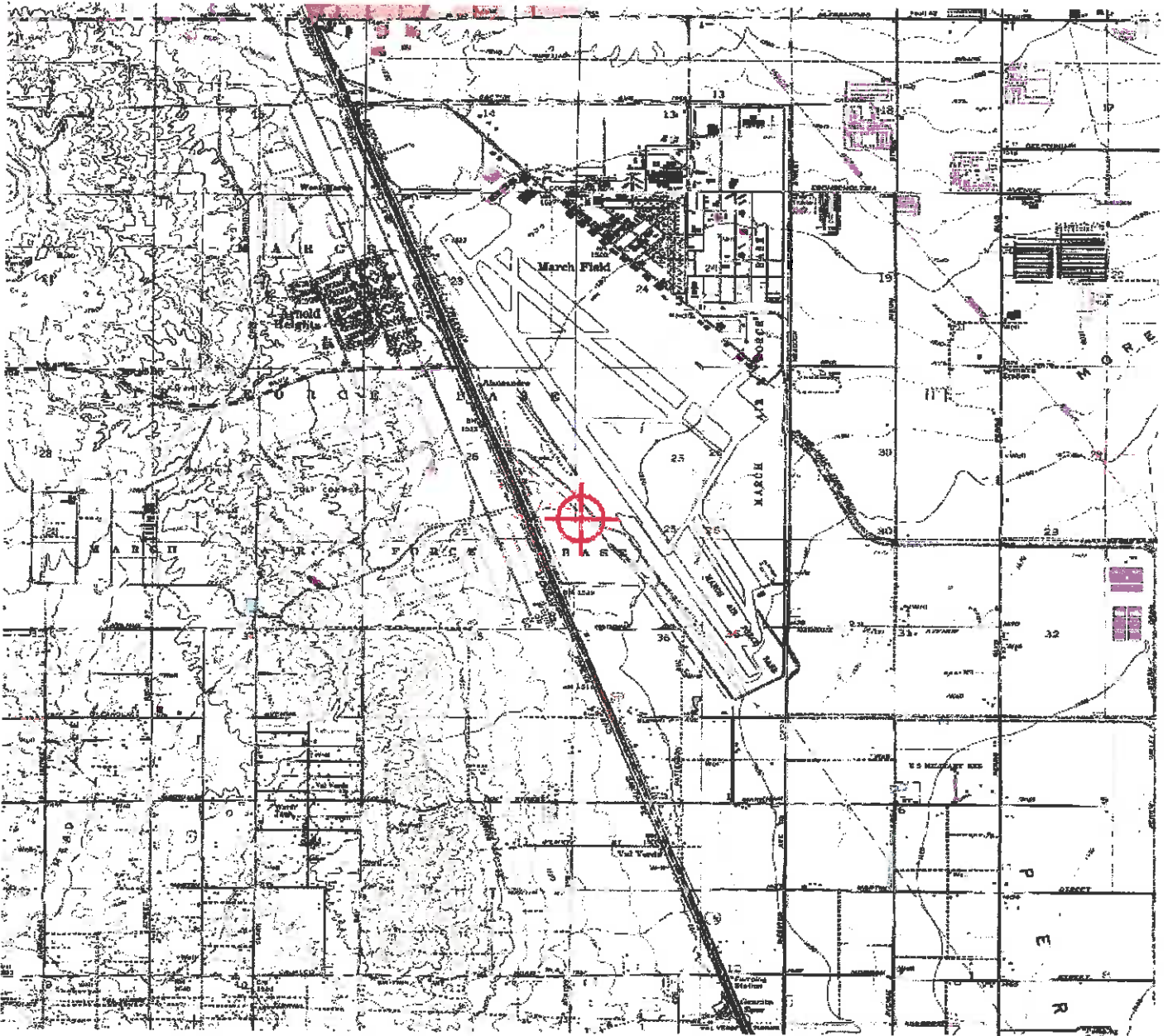
(EXT)

Attachment(s)
Additional Information
Map(s)

Additional information for ASN 2016-AWP-12034-OE

ALL REQUIREMENTS AND CONDITIONS ON ORIGINAL AIRSPACE DETERMINATION REMAIN IN EFFECT.

TOPO Map for ASN 2016-AWP-12034-OE



TOPO Map for ASN 2016-AWP-12034-OE



Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2016-AWP-12035-OE

Issued Date: 01/26/2017

JOHN MAGNESS
 RIVERSIDE INLAND DEVELOPMENT, LLC
 901 VIA PIEMONTE
 SUITE 175
 ONTARIO, CA 91764

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building VIP - Bldg 2 (SW Corner)
 Location: Perris, CA
 Latitude: 33-52-14.19N NAD 83
 Longitude: 117-15-37.40W
 Heights: 1508 feet site elevation (SE)
 48 feet above ground level (AGL)
 1556 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 1.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 07/26/2018 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.

- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates , heights, frequency(ies) and power . Any changes in coordinates , heights, and frequencies or use of greater power will void this determination. Any future construction or alteration , including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (310) 725-6557. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2016-AWP-12035-OE.

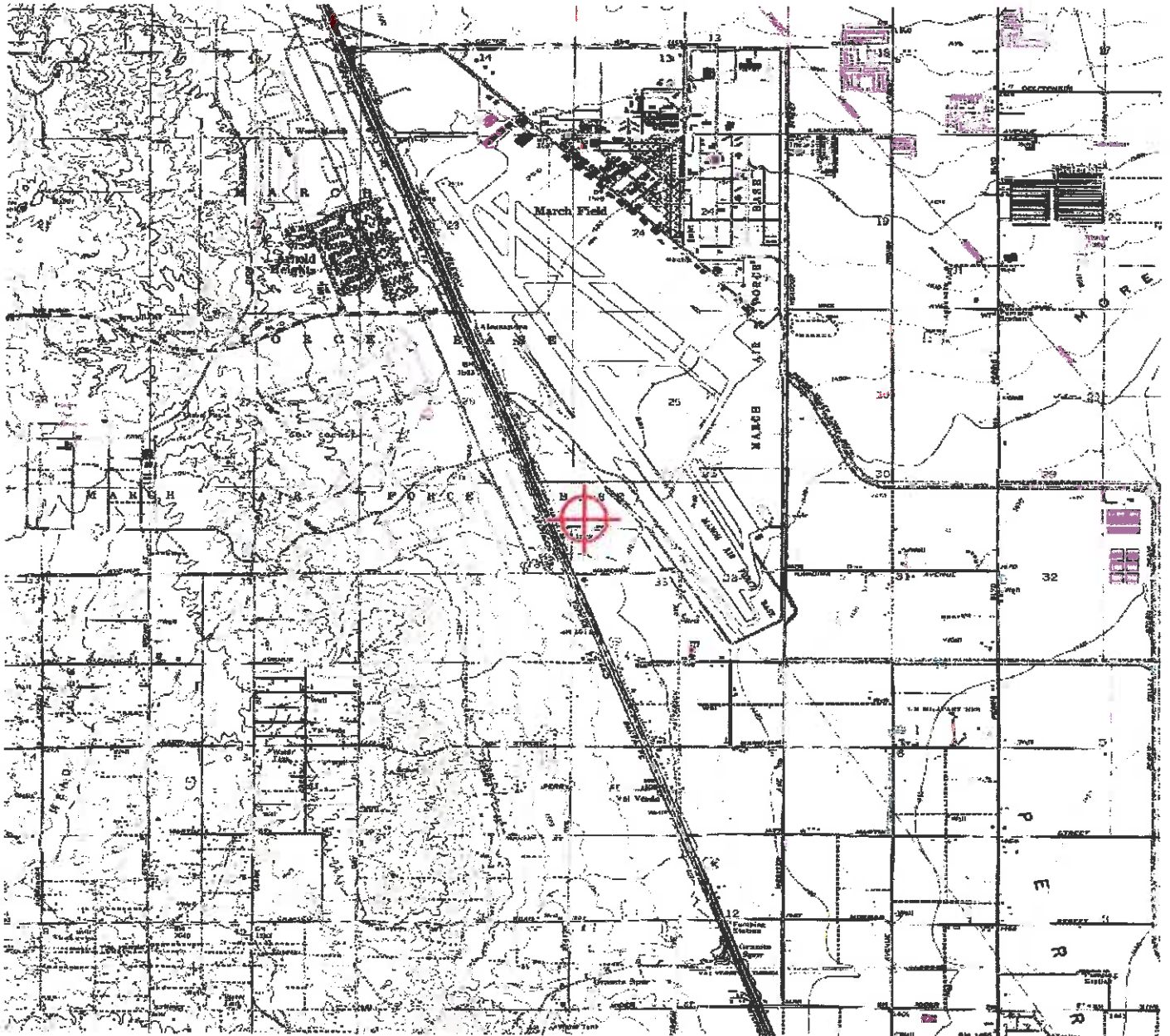
Signature Control No: 311816040-320352865

(DNE)

Karen McDonald
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2016-AWP-12035-OE





Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2016-AWP-12035-OE

Issued Date: 08/21/2018

JOHN MAGNESS
RIVERSIDE INLAND DEVELOPMENT, LLC
901 VIA PIEMONTE
SUITE 175
ONTARIO, CA 91764

**** Extension ****

A Determination was issued by the Federal Aviation Administration (FAA) concerning:

Structure:	Building VIP - Bldg 2 (SW Corner)
Location:	Perris, CA
Latitude:	33-52-14.19N NAD 83
Longitude:	117-15-37.40W
Heights:	1508 feet site elevation (SE) 48 feet above ground level (AGL) 1556 feet above mean sea level (AMSL)

In response to your request for an extension of the effective period of the determination, the FAA has reviewed the aeronautical study in light of current aeronautical operations in the area of the structure and finds that no significant aeronautical changes have occurred which would alter the determination issued for this structure.

Accordingly, pursuant to the authority delegated to me, the effective period of the determination issued under the above cited aeronautical study number is hereby extended and will expire on 02/21/2020 unless otherwise extended, revised, or terminated by this office. You must adhere to all conditions identified in the original determination.

This extension issued in accordance with 49 U.S.C., Section 44718 and, if applicable, Title 14 of the Code of Federal Regulations, part 77, concerns the effect of the structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (310) 725-6557, or karen.mcdonald@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2016-AWP-12035-OE.

Signature Control No: 311816040-382228445
Karen McDonald
Specialist

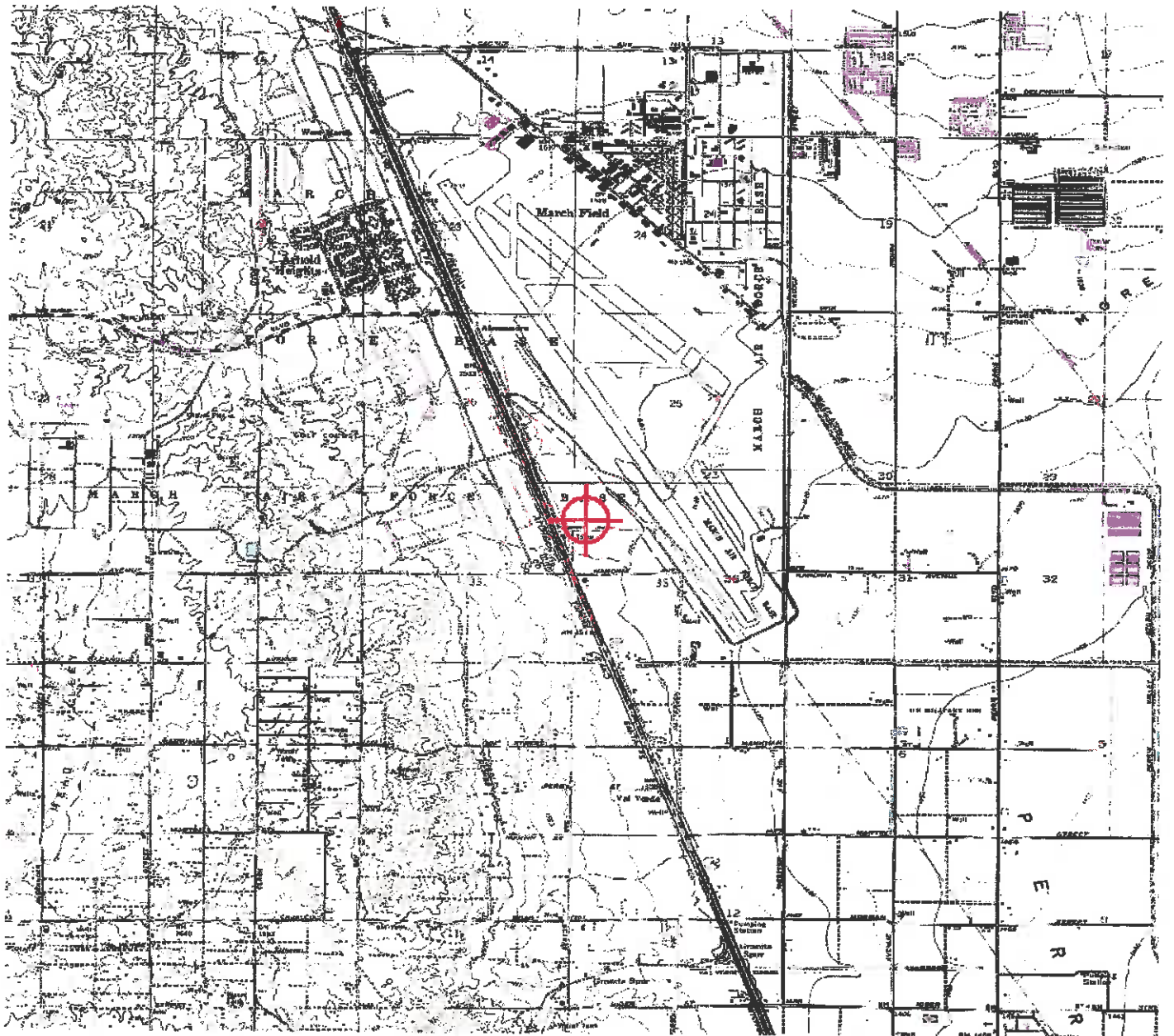
(EXT)

Attachment(s)
Additional Information
Map(s)

Additional information for ASN 2016-AWP-12035-OE

ALL REQUIREMENTS AND CONDITIONS ON ORIGINAL AIRSPACE DETERMINATION REMAIN IN EFFECT.

TOPO Map for ASN 2016-AWP-12035-OE





Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2016-AWP-12036-OE

Issued Date: 01/26/2017

JOHN MAGNESS
 RIVERSIDE INLAND DEVELOPMENT, LLC
 901 VIA PIEMONTE
 SUITE 175
 ONTARIO, CA 91764

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building VIP - Bldg 2 (SE Corner)
 Location: Perris, CA
 Latitude: 33-52-16.33N NAD 83
 Longitude: 117-15-28.81W
 Heights: 1508 feet site elevation (SE)
 48 feet above ground level (AGL)
 1556 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does exceed obstruction standards but would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

As a condition to this Determination, the structure is to be marked/lighted in accordance with FAA Advisory circular 70/7460-1 L Change 1, Obstruction Marking and Lighting, red lights - Chapters 4,5(Red),&12.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 07/26/2018 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates , heights, frequency(ies) and power . Any changes in coordinates , heights, and frequencies or use of greater power will void this determination. Any future construction or alteration , including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (310) 725-6557. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2016-AWP-12036-OE.

Signature Control No: 311816041-320354780
Karen McDonald
Specialist

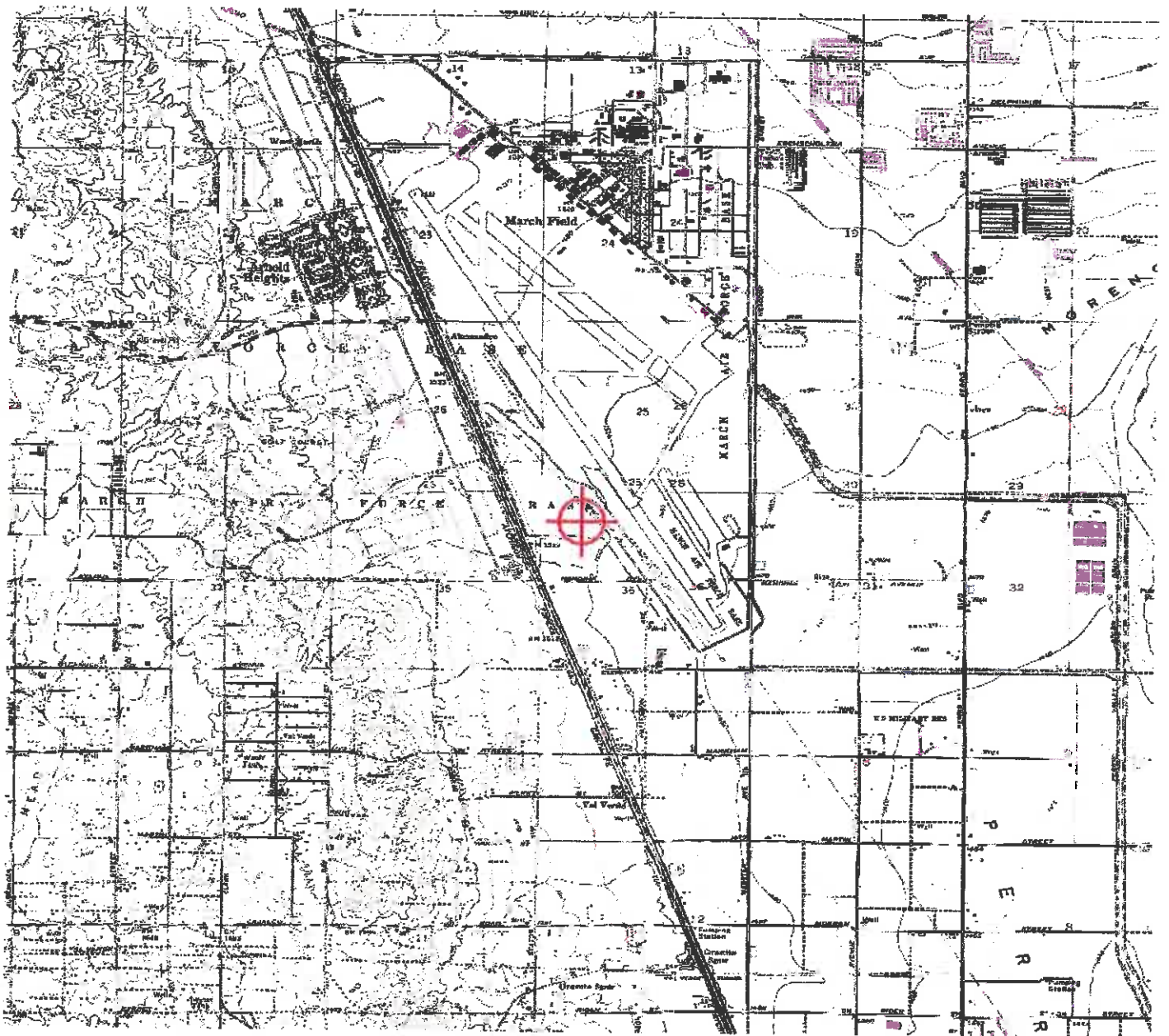
(EBO)

Attachment(s)
Additional Information
Map(s)

Additional information for ASN 2016-AWP-12036-OE

RECOMMEND RED OBSTRUCTION LIGHTING INSTALLED ON THIS POINT OF BUILDING. HEIGHT PENETRATES AN FAR PART 77 PROTECTED SURFACE; 77.21(b)(4) *RIV[EXISTING][NASR] - RWY 14/32[EXISTING][NASR]: Transitional Surface ---> Exceeds by 1 ft.

TOPO Map for ASN 2016-AWP-12036-OE





Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2016-AWP-12036-OE

Issued Date: 08/21/2018

JOHN MAGNESS
RIVERSIDE INLAND DEVELOPMENT, LLC
901 VIA PIEMONTE
SUITE 175
ONTARIO, CA 91764

**** Extension ****

A Determination was issued by the Federal Aviation Administration (FAA) concerning:

Structure:	Building VIP - Bldg 2 (SE Corner)
Location:	Perris, CA
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Longitude:	117-15-28.81W
Heights:	1508 feet site elevation (SE) 48 feet above ground level (AGL) 1556 feet above mean sea level (AMSL)

In response to your request for an extension of the effective period of the determination, the FAA has reviewed the aeronautical study in light of current aeronautical operations in the area of the structure and finds that no significant aeronautical changes have occurred which would alter the determination issued for this structure.

Accordingly, pursuant to the authority delegated to me, the effective period of the determination issued under the above cited aeronautical study number is hereby extended and will expire on 02/21/2020 unless otherwise extended, revised, or terminated by this office. You must adhere to all conditions identified in the original determination.

This extension issued in accordance with 49 U.S.C., Section 44718 and, if applicable, Title 14 of the Code of Federal Regulations, part 77, concerns the effect of the structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (310) 725-6557, or karen.mcdonald@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2016-AWP-12036-OE.

Signature Control No: 311816041-382228443

Karen McDonald
Specialist

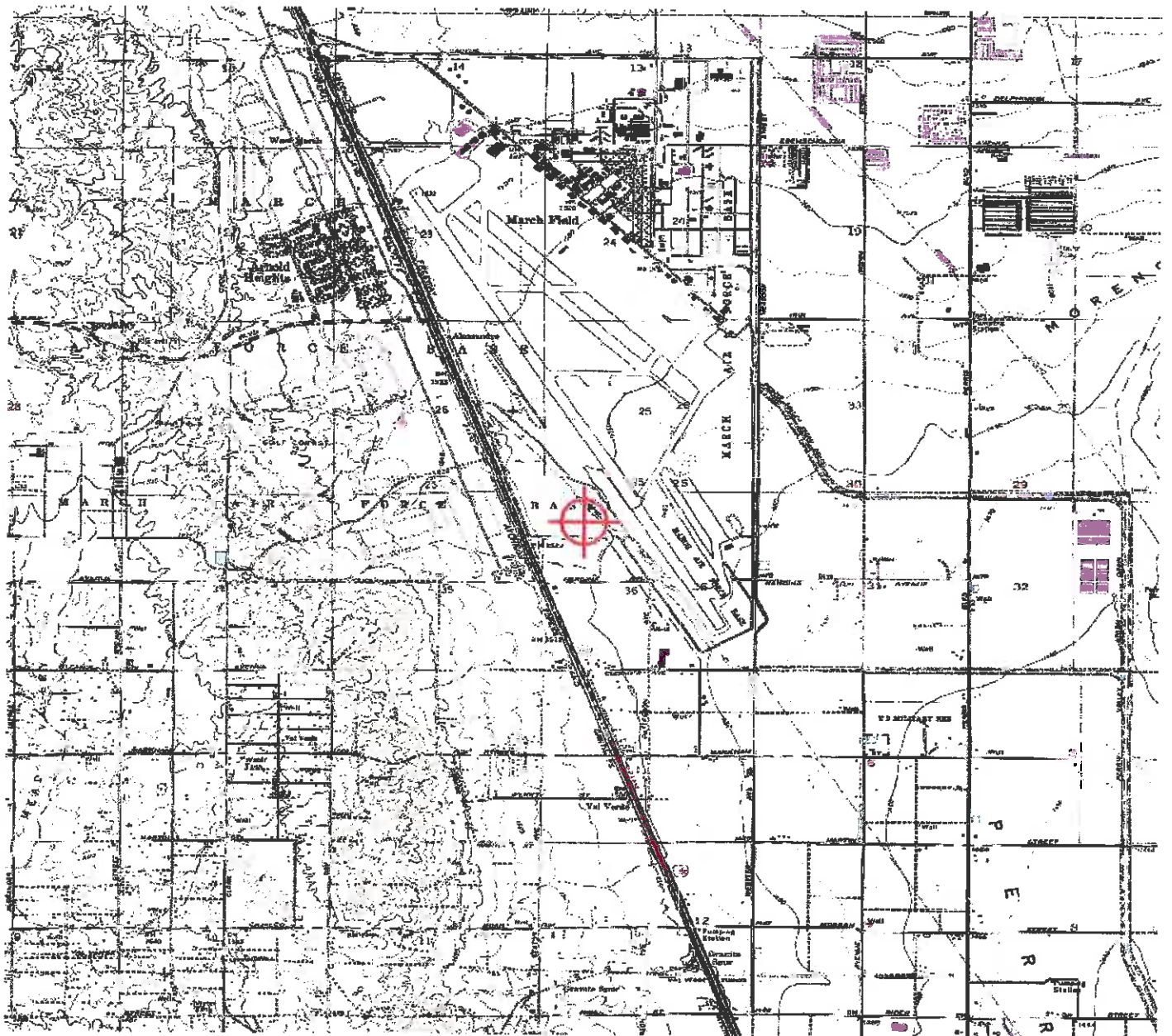
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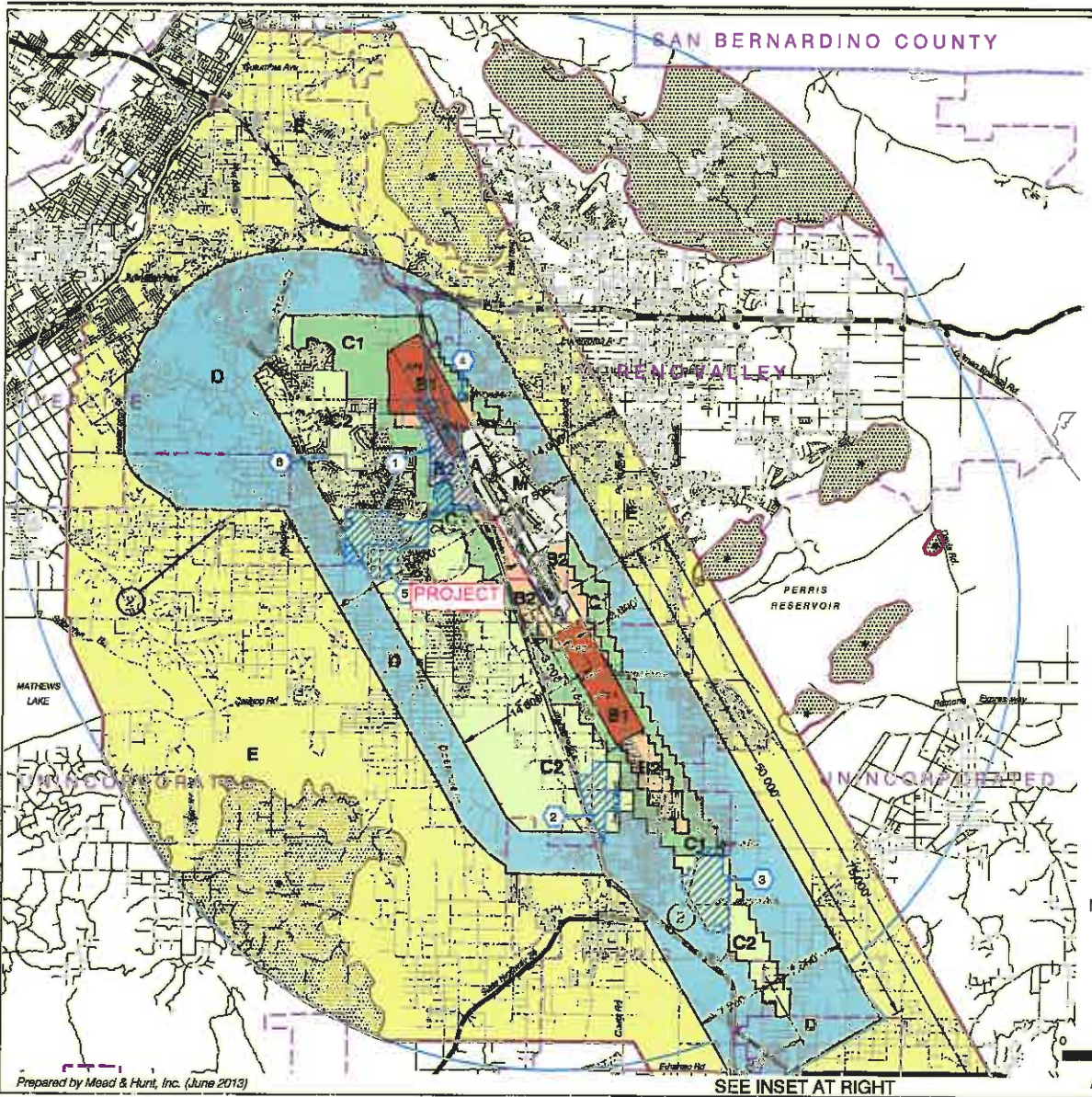
Attachment(s)
Additional Information
Map(s)

Additional information for ASN 2016-AWP-12036-OE

ALL REQUIREMENTS AND CONDITIONS ON ORIGINAL AIRSPACE DETERMINATION REMAIN IN EFFECT.

TOPO Map for ASN 2016-AWP-12036-OE





LEGEND

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

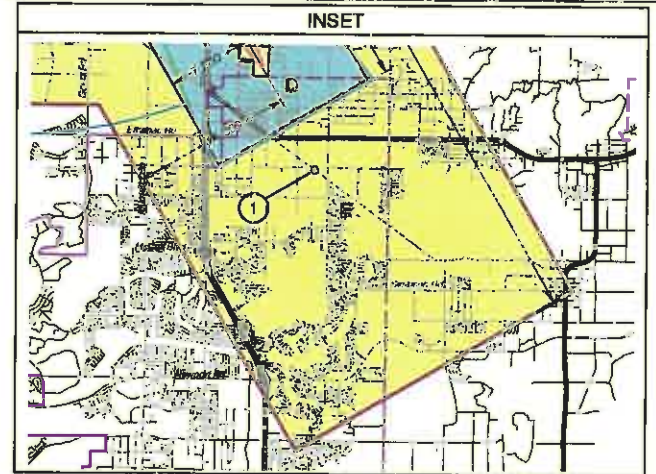
Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- ▭ Site-Specific Exceptions (existing local agency commitments to development projects)

① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.

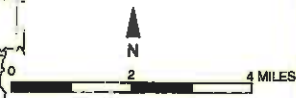
② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



Riverside County
 Airport Land Use Commission
 March Air Reserve Base / Inland Port Airport
 Land Use Compatibility Plan
 (Adopted November 13, 2014)

Note:
 All dimensions are measured from
 runway ends and centerlines.



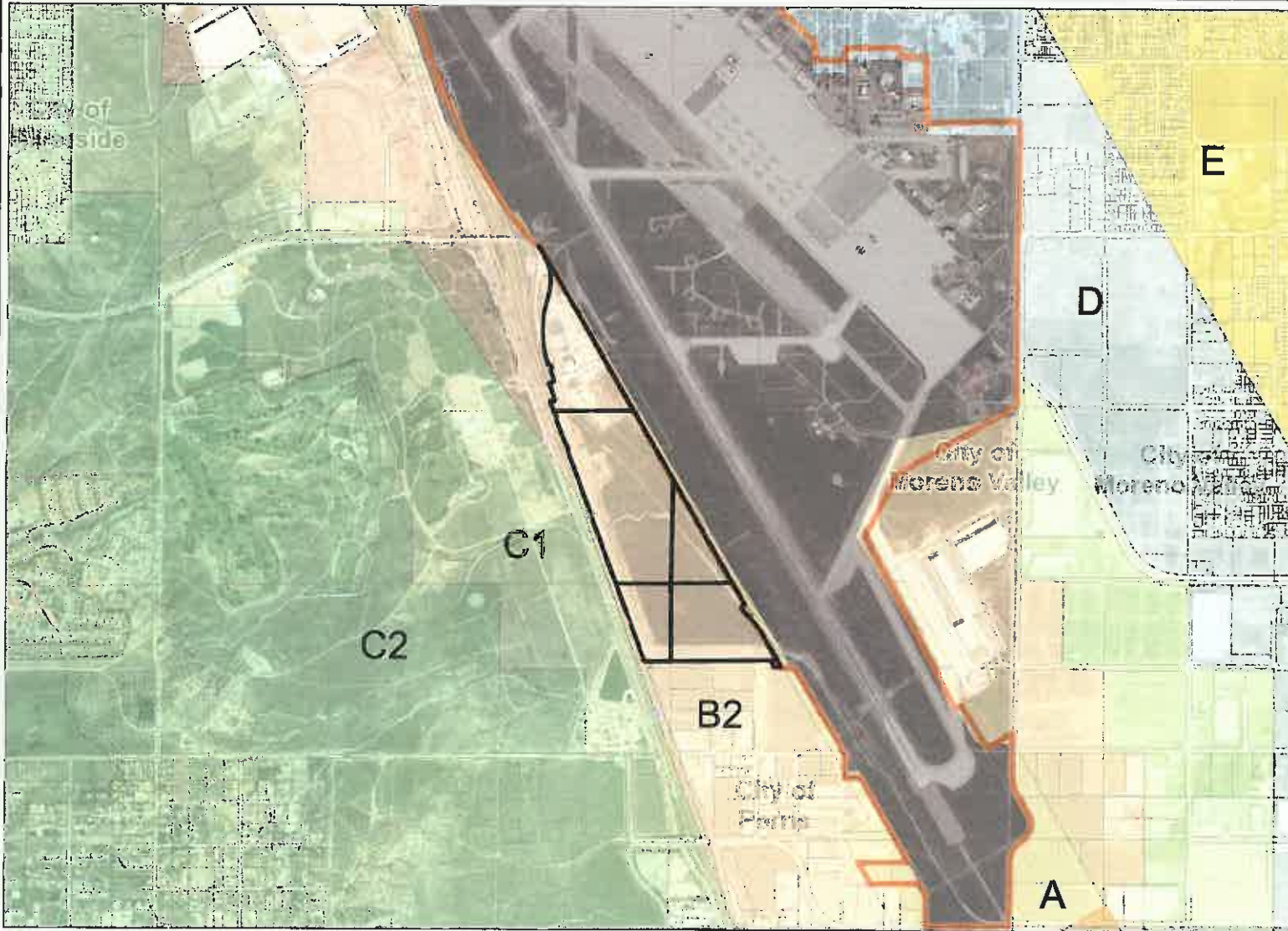
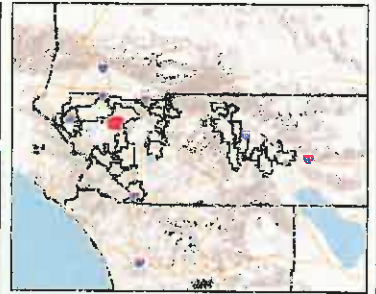
Base map source: County of Riverside 2013

Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map MA-1
Compatibility Map
 March Air Reserve Base / Inland Port Airport

My Map



Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC8
- C2-HIGHT



0 2,877 5,755 Feet



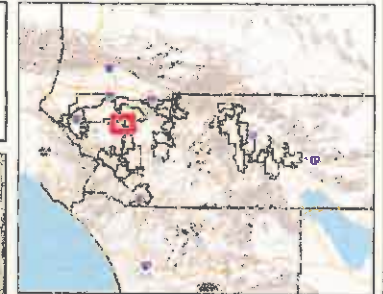
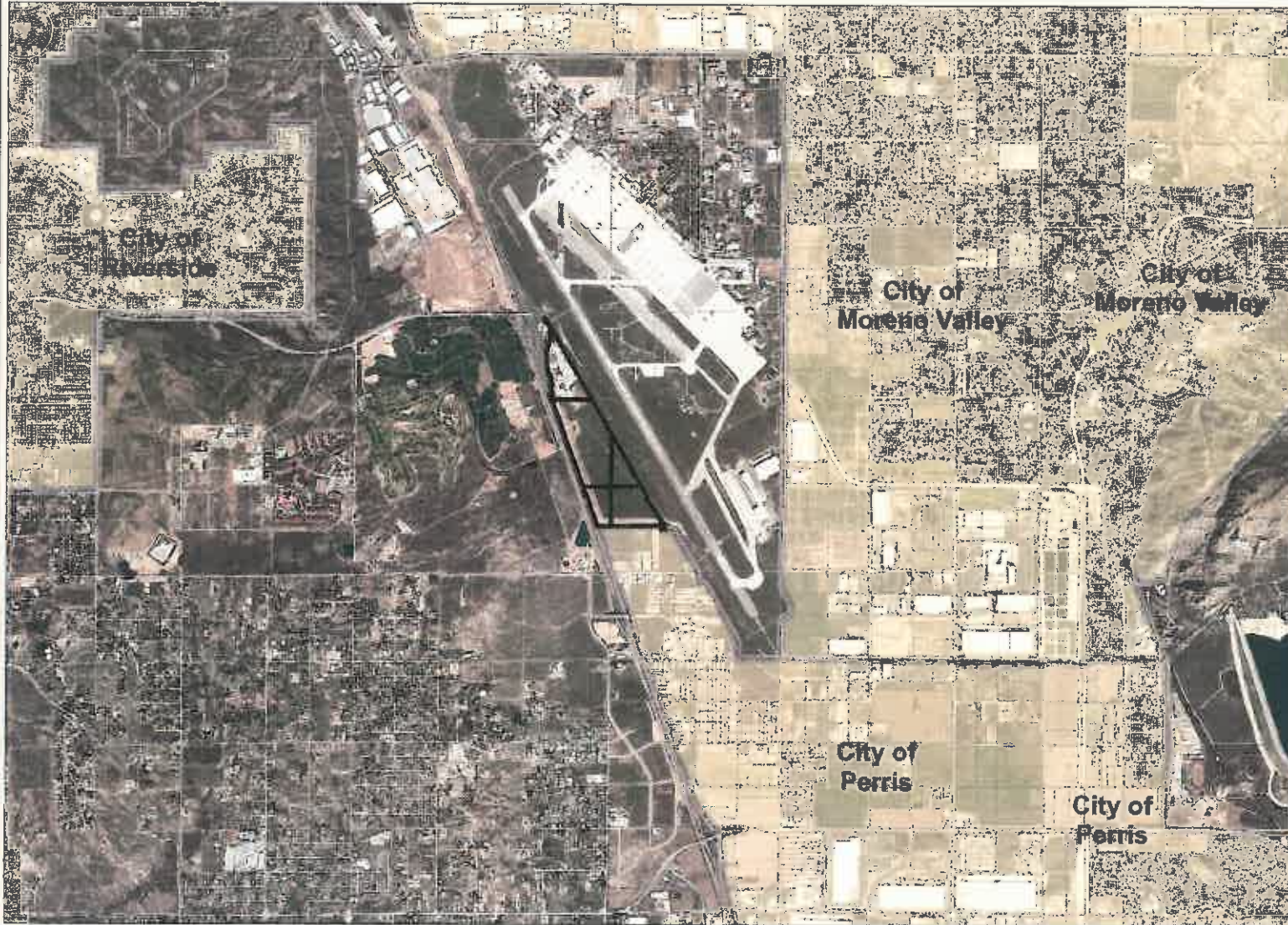
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON...6/30/2017 1:48:34 PM

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Notes

My Map



Legend

- City Boundaries
- Cities
- highways_large
 - HWY
 - INTERCHANGE
 - INTERSTATE
 - USHWY
- majorroads
- counties
- cities
- hydrographylines
- waterbodies
 - Lakes
 - Rivers



0 5,755 11,510 Feet



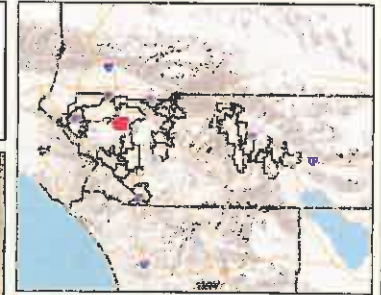
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REPORT PRINTED ON... 6/30/2017 1:51:48 PM

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Notes

My Map



Legend

- City Boundaries
- Cities
- roads
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities
- hydrography
- lines
- waterbodies
- Lakes
- Rivers



0 2,877 5,755 Feet



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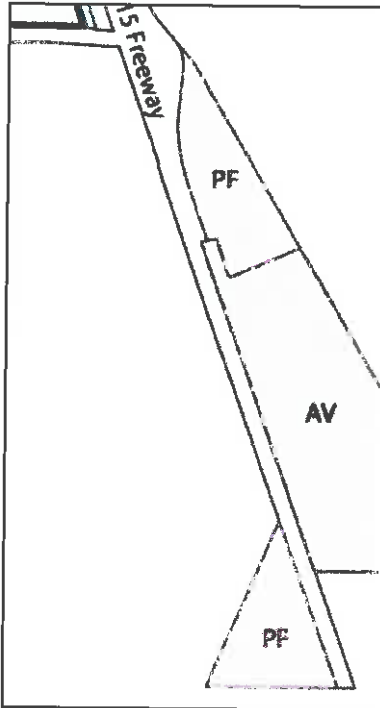
© Riverside County RCIT GIS

Notes

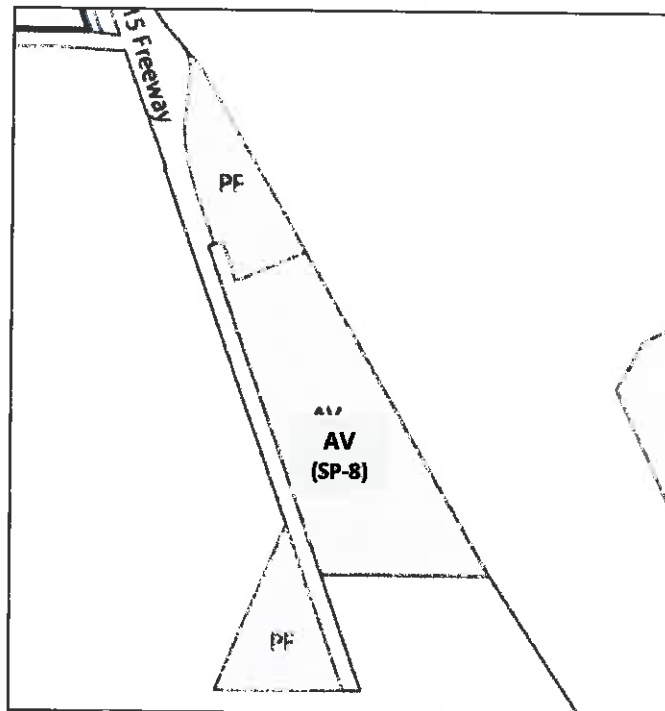
Veterans Industrial Park Specific Plan
General Plan Text Amendment (Redline additions noted)

Aviation (AV) (FAR 0.40)

Through joint use of the aviation field, the unique opportunity to develop civilian aviation is presented. Land uses under this designation include flightline, hangers, aviation support services, inclusive of fuel systems and dispensing, air cargo storage, passenger and air cargo terminals, fixed base operations, aircraft maintenance and aviation operation services as well as general warehousing/logistics uses. The maximum FAR for land use in this category is 0.40:1, with an average development intensity of 0.15:1.



Existing General Plan Land Use



Proposed General Plan Amendment

General Plan Amendment Summary

Veterans Industrial Park Specific Plan

1. **LAND USE ELEMENT.** Land Use Plan: Map Amendment to add a Specific Plan (SP) overlay to the existing Aviation Land Use and a text amendment to add general warehousing/logistics to the list of uses in the Aviation designation.
 - No Amendments to Exhibits 1-1 through 1-3
 - Amendment to Exhibit 1-4 (Land Use Plan) to add (SP) to the Aviation designation.
 - Update Table 1-1 to include updated Buildout Capacity inclusive of proposed project intensity if necessary.
2. **TRANSPORTATION ELEMENT.** Van Buren Boulevard Extension is identified as a Major Arterial on the General Plan Transportation Plan Exhibit 2-1. A Major Arterial has 4 lanes and a right of way of 110 feet with an 86-foot road section and 18 foot raised median. The proposed road section provides a modified Major Arterial.

No bike lanes are identified for Van Buren east of I-215. Van Buren, as a road of a higher classification than a collector, is by default identified as a truck route (Exhibit 2-3).

 - No Amendment to Exhibit 2-1 (Transportation Plan)
 - No Amendment to Exhibit 2-2 (Cross Sections)
 - No Amendment to Exhibit 2-3 (Transportation Plan Systems)
 - No Amendment to Exhibit 2-4 (Non-Roadway Transportation Plan Systems)
3. **RESOURCE MANAGEMENT ELEMENT.** Site is within the SKR Open Space but not the SKR Management Area nor shown for recreational facilities. Van Buren east of I-215 is not designated on General Plan exhibits as a scenic corridor.
 - No Amendment to Exhibit 5-1 (SKR Management and Open Space Areas). SKR mitigation fees will be paid.
 - No Amendment to Exhibit 5-2 (Wetlands and Riparian Areas)
 - No Amendment to Exhibit 5-3 (Recreational Facilities)
 - No Amendment to Exhibit 5-4 (Scenic Corridors/Gateway)
 - No Amendment to Exhibit 5-5 (Installation Restoration Program Sites)
4. **SAFETY/RISK MANAGEMENT ELEMENT.** Site is not within a Clear Zone, APZ or RPZ as shown on General Plan exhibits.

- **No Amendment to Exhibit 6-1 (Seismic setting)**
- **No Amendment to Exhibit 6-2 (100-Year Flood Area)**
- **No Amendment to Exhibit 6-3 (Aviation Safety Zones (Military))**
- **No Amendment to Exhibit 6-4 (Aviation Safety Zones (Civilian))**

LEGAL DESCRIPTIONS

THE LEGAL DESCRIPTION SHOWN HEREON HAS BEEN PREPARED BY THE SURVEYOR OF RECORDS FOR THE PURPOSE OF REFLECTING THE AREA AS BEING SURVEYED IN THIS SURVEY. ONLY, SAID DESCRIPTION IS NOT TO BE USED FOR CONVEYANCE OF TITLE OF ANY NATURE.

THAT PORTION SECTIONS 23, 25, 26 AND 30, TOWNSHIP 3 SOUTH, RANGE 4 WEST, SAN BERNARDINO MOUNTAIN IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA SHOWN AS PARCEL 1 ON MAP FILED IN BOOK 110 PAGES 30 TO 40 INCLUSIVE OF RECORDS OF SURVEY IN THE OFFICE OF THE COUNTY RECORDER, OF SAID COUNTY, DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHERLY TERMINUS OF THAT CERTAIN COURSE SHOWN AS HAVING A BEARING AND DISTANCE OF NORTH 30°05'00" WEST 870.28 FEET IN THE EASTELY BOUNDARY OF SAID PARCEL 11, SAID SOUTHERLY TERMINUS ALSO BEING A POINT ON THE NORTHERLY LINE OF PARCEL MAP NO. 8848, AS NOW MAP FILED IN BOOK 37 PAGE 90, OF PARCEL MAPS IN THE OFFICE OF SAID RECORDER; THENCE ALONG SAID NORTHERLY LINE SOUTH 89°53'52" WEST 117.66 FEET; THENCE LEAVING SAID NORTHERLY LINE NORTH 80°29'25" WEST 134.78 FEET TO A LINE PARALLEL WITH 100.00 FEET NORTHERLY, MEASURED AT RIGHT ANGLES FROM SAID NORTHERLY LINE; THENCE ALONG SAID PARALLEL LINE SOUTH 80°24'52" WEST 855.75 FEET TO THE EASTELY BOUNDARY OF CALIFORNIA STATE ROUTE 215, AS SHOWN ON CALIFORNIA DEPARTMENT OF TRANSPORTATION INDEMNIFICATION MAP 42880 ON FILE IN THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION; THENCE ALONG SAID EASTELY BOUNDARY NORTH 17°22'43" WEST 4259.33 FEET; THENCE LEAVING SAID EASTELY BOUNDARY NORTH 77°50'00" EAST 1202.33 FEET; THENCE SOUTH 17°00'00" EAST 24.72 FEET; THENCE NORTH 70°47'41" EAST 81.83 FEET; THENCE SOUTH 42°27'50" EAST 765.67 FEET; THENCE NORTH 84°40'00" EAST 814.81 FEET TO THE NORTHEASTERLY TERMINUS OF THAT CERTAIN COURSE SHOWN AS HAVING A BEARING AND DISTANCE OF NORTH 30°02'27" WEST 3059.80 FEET IN THE BOUNDARY OF THE "MARCH AIR RESERVE BASE" AS SHOWN ON MAP FILED IN BOOK 124 PAGES 69 TO 70 INCLUSIVE OF SAID RECORDS OF SURVEY; THENCE ALONG SAID EAST MENTIONED BOUNDARY AS FOLLOWS: SOUTH 30°02'27" EAST 3027.67 FEET; THENCE SOUTH 42°42'00" WEST 73.50 FEET; THENCE SOUTH 39°17'40" EAST 4214.33 FEET; THENCE SOUTH 30°29'20" EAST 870.23 FEET TO THE POINT OF BEGINNING.

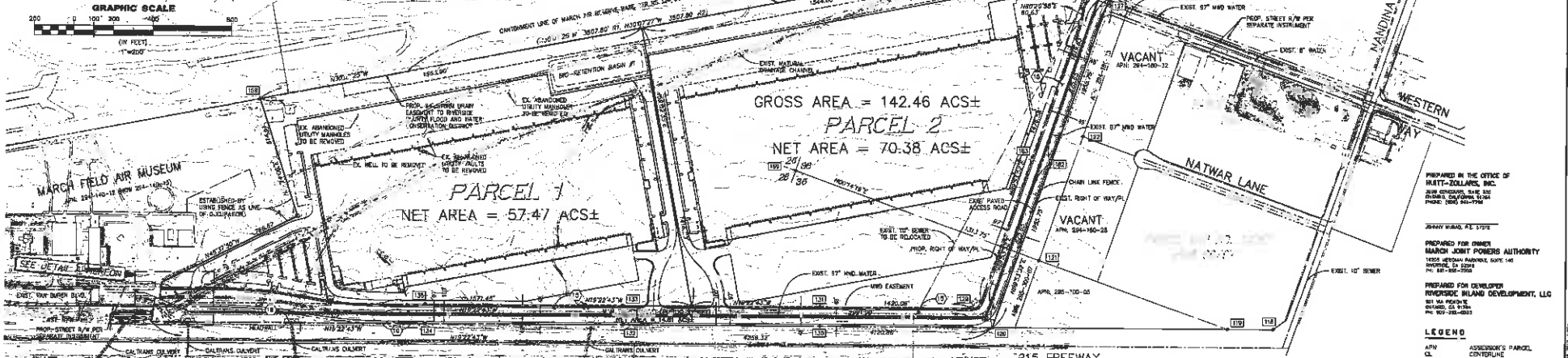
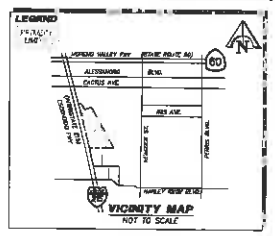
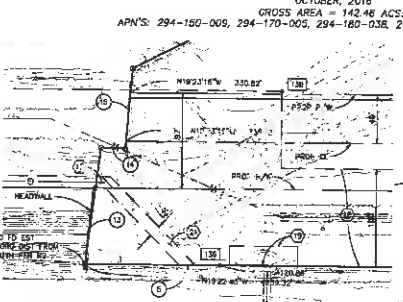
CONTAINING 142.46 ACRES MORE OR LESS.

EXISTING EASEMENTS

- 16 ABUTMENT RIGHTS OF INGRESS AND EGRESS TO OR FROM THE FREEWAY ARE BEING REAFFIRMED IN THE DOCUMENT RECORDED OCTOBER 28, 2004 AS INSTRUMENT NO. 2004-032638 OF OFFICIAL RECORDS.
- 17 AN EASEMENT FOR PIPELINE AND INCIDENTAL PURPOSES, RECORDED JUNE 26, 2009 AS INSTRUMENT NO. 2009-0326343 OF OFFICIAL RECORDS.
- 18 IN FAVOR OF THE METROPOLITAN WATER DISTRICT OF SOUTHERN CALIFORNIA AS DESCRIBED THEREIN.
- 19 AN EASEMENT FOR PUBLIC ROAD AND UTILITY AND INCIDENTAL PURPOSES, RECORDED AUGUST 10, 2011 AS INSTRUMENT NO. 2011-0306013 AND RE-RECORDED AUGUST 28, 2012 AS INSTRUMENT NO. 2012-0304841, BOTH OF OFFICIAL RECORDS.
- 20 IN FAVOR OF THE COUNTY OF RIVERSIDE AS DESCRIBED THEREIN.
- 21 AN EASEMENT FOR PUBLIC ROAD AND UTILITY AND INCIDENTAL PURPOSES, RECORDED AUGUST 10, 2011 AS INSTRUMENT NO. 2011-0306013 OF OFFICIAL RECORDS.
- 22 IN FAVOR OF THE STATE OF CALIFORNIA AS DESCRIBED THEREIN.
- 23 AN EASEMENT FOR PIPELINE AND INCIDENTAL PURPOSES, RECORDED JUNE 27, 2013 AS INSTRUMENT NO. 2013-0307855 OF OFFICIAL RECORDS.
- 24 IN FAVOR OF THE SOUTHERN CALIFORNIA GAS COMPANY AS DESCRIBED THEREIN.

TENTATIVE PARCEL MAP NO. 37220

COUNTY OF RIVERSIDE, STATE OF CALIFORNIA
 HUITT-ZOLLARS, INC.
 OCTOBER, 2016
 GROSS AREA = 142.46 AC±
 APNs: 294-150-009, 294-170-005, 294-180-038, 294-140-113, 295-300-008



GENERAL NOTES

1. BASIS OF BEARING: THE BASIS OF BEARINGS FOR THIS SURVEY IS THE CALIFORNIA COORDINATE SYSTEM, ZONE 8, NAD 83 (EPOCH 2011.00) AS DETERMINED LOCALLY BY A LINE BETWEEN COORDINATE STATIONING STATIONS (COPS) AND FOR CONTINUOUS OPERATING REFERENCE STATIONS (CORS) TRIP AND PERSI (CORS 8 48° 12'34.02" E AS CORN 20) FROM GEODETIC VALUES PUBLISHED BY THE CALIFORNIA SPATIAL REFERENCE CENTER (CSRC) AND/OR NATIONAL GEODETIC SURVEY (NGS), RESPECTIVELY. ALL DISTANCES ARE U.S. FEET (ROUND). CURVED DISTANCES CAN BE OBTAINED BY MULTIPLYING A CURVATURE FACTOR OF 0.00003037.
2. THE BENCHMARK FOR THIS PROJECT IS BASED ON THE NATIONAL GEODETIC SURVEY (NGS) VERTICAL CONTROL NETWORK. BENCHMARK NUMBER PD 02222 DESIGNATION 2143, ELEVATION 1532.70 (NAVD83), DESCRIBED AS FOLLOWS: "NORTH VALLEY, NEAR THE INTERSECTION OF INTERSTATE 215 AND VAN BUREN BLVD. PROCEED WEST ALONG VAN BUREN BLVD TO AVE. A (WEST FRONTAGE RD) THEN 0.10 MILE (0.16 KM) NORTH ALONG AVE. A TO A DIRT ROAD LEADING TO THE UNDER PASS OF VAN BUREN BLVD. 0.35 MILE (0.56 KM) SOUTH ALONG DIRT ROAD, ON THE EAST SIDE OF A TYPICAL HIGHWAY TRACK; FOUND A 1/2\"/>

POINT DESCRIPTIONS

- 118 FOUND CALIF. TRANS. CONCA'S BOX DO IN 0.5' PER R3 RECORDS OF SURVEY 124/89/100 HELD FOR USE ONLY
- 119 FOUND 1\"/>

COURSES

- 1 NORTH 49°46'59\"/>

UTILITY PERMITS

- WE-H/PT: GROSS MEASURED WATER (GROSS) 1/2\"/>

WE-H/W: WATER MEASURED WATER (GROSS) 1/2\"/>

WE-H/S: SLOTTED WATER (GROSS) 1/2\"/>

WE-H/D: DRAINAGE (GROSS) 1/2\"/>

WE-H/E: EGRESS (GROSS) 1/2\"/>

WE-H/F: FLOOD (GROSS) 1/2\"/>

WE-H/G: GAS (GROSS) 1/2\"/>

WE-H/H: HIGH VOLTAGE (GROSS) 1/2\"/>

WE-H/I: INSULATION (GROSS) 1/2\"/>

WE-H/J: JUNCTION (GROSS) 1/2\"/>

WE-H/K: KILN (GROSS) 1/2\"/>

WE-H/L: LAMP (GROSS) 1/2\"/>

WE-H/M: METER (GROSS) 1/2\"/>

WE-H/N: NEST (GROSS) 1/2\"/>

WE-H/O: OIL (GROSS) 1/2\"/>

WE-H/P: PUMP (GROSS) 1/2\"/>

WE-H/Q: QUANTITY (GROSS) 1/2\"/>

WE-H/R: RADIATION (GROSS) 1/2\"/>

WE-H/S: SINK (GROSS) 1/2\"/>

WE-H/T: TANK (GROSS) 1/2\"/>

WE-H/U: UTILITY (GROSS) 1/2\"/>

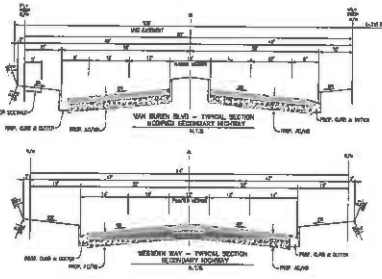
WE-H/V: VALVE (GROSS) 1/2\"/>

WE-H/W: WAREHOUSE (GROSS) 1/2\"/>

WE-H/X: X-RAY (GROSS) 1/2\"/>

WE-H/Y: YARD (GROSS) 1/2\"/>

WE-H/Z: ZONE (GROSS) 1/2\"/>



REV	DESCRIPTION	DATE
1	ISSUED FOR PERMITS	10/10/16
2	ISSUED FOR PERMITS	10/10/16
3	ISSUED FOR PERMITS	10/10/16
4	ISSUED FOR PERMITS	10/10/16
5	ISSUED FOR PERMITS	10/10/16
6	ISSUED FOR PERMITS	10/10/16
7	ISSUED FOR PERMITS	10/10/16
8	ISSUED FOR PERMITS	10/10/16
9	ISSUED FOR PERMITS	10/10/16
10	ISSUED FOR PERMITS	10/10/16

TENTATIVE PARCEL MAP 37220
 FOR
VETERANS INDUSTRIAL PARK 215
 EAST OF 316 FREEWAY
 COUNTY OF RIVERSIDE

HUITT-ZOLLARS
 REGISTERED PROFESSIONAL SURVEYORS
 2150 CENTRAL AVENUE, SUITE 300 • RIVERSIDE, CALIFORNIA 92504 • (951) 514-7700
 PREPARED BY: JERRY WANG
 DATE: 10/10/16

DATE OF MAP	10/10/16
DATE OF SURVEY	10/10/16
DATE OF STAFF	10/10/16
DATE OF RECORD	10/10/16
DATE OF FILE	10/10/16
DATE OF PRINT	10/10/16

- LEGEND**
- APN ADDRESSOR'S PARCEL
 - CL CENTERLINE
 - CONC CONCRETE
 - DRY DRAINAGE
 - ELEV ELEVATION
 - EXIST EXISTING
 - FOOT FOOTING
 - HD NORTH
 - NELY NORTH-EASTERLY
 - NWLY NORTH-WESTERLY
 - SELY SOUTH-EASTERLY
 - SWLY SOUTH-WESTERLY
 - WELY WESTERLY
 - MONUMENT FOUND AS NOTED
 - WARRANT TO BE SET
 - SUR. PT. POINT NUMBER
 - DRAINAGE FLOW DIRECTION

- REFERENCES**
- R1 = RECORDS OF SURVEY 110/20-10
 - R2 = RECORDS OF SURVEY 124/89-01
 - R3 = PARCEL MAP NO. 8848, PMS 37/90
 - R4 = RECORDS OF SURVEY 140/47
 - R5 = RECORDS OF SURVEY 135/97-100
 - R6 = INSTRUMENT NO. 2010-0305043 RECORDED 11/21/2010
 - R7 = CALIFORNIA DEPT. OF TRANSPORTATION INDEMNIFICATION MAP NO. 42880 (RIVERSIDE COUNTY MAP 200-1343) RECORDED 04/26/2009
 - R8 = INSTRUMENT NO. 2006-0326343 RECORDED 06/26/2006

STANDARD JPA NOTES

1. ALL NEW EXISTING UTILITIES SHALL BE DEEPENED TO MEET THE REQUIREMENTS OF THE SITE AND BE PROTECTED FROM DAMAGE.
2. ALL EXISTING UTILITIES SHALL BE DEEPENED TO MEET THE REQUIREMENTS OF THE SITE AND BE PROTECTED FROM DAMAGE.
3. ALL EXISTING UTILITIES SHALL BE DEEPENED TO MEET THE REQUIREMENTS OF THE SITE AND BE PROTECTED FROM DAMAGE.
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34. ALL EXISTING UTILITIES SHALL BE DEEPENED TO MEET THE REQUIREMENTS OF THE SITE AND BE PROTECTED FROM DAMAGE.
35. ALL EXISTING UTILITIES SHALL BE DEEPENED TO MEET THE REQUIREMENTS OF THE SITE AND BE PROTECTED FROM DAMAGE.

LEGAL DESCRIPTION

THE LEGAL DESCRIPTION HEREIN HAS BEEN PREPARED BY THE SURVEYOR OF RECORD FOR THE PURPOSE OF RECORDING THE AREA TO BE INCLUDED IN THIS SURVEY ONLY AND DOES NOT CONSTITUTE A WARRANTY OF TITLE OR ANY OTHER WARRANTY.

THE LEGAL DESCRIPTION IS BASED UPON THE RECORDS OF THE COUNTY OF RIVERSIDE, CALIFORNIA, AND THE RECORDS OF THE COUNTY OF SAN DIEGO, CALIFORNIA.

THE LEGAL DESCRIPTION IS BASED UPON THE RECORDS OF THE COUNTY OF RIVERSIDE, CALIFORNIA, AND THE RECORDS OF THE COUNTY OF SAN DIEGO, CALIFORNIA.

THE LEGAL DESCRIPTION IS BASED UPON THE RECORDS OF THE COUNTY OF RIVERSIDE, CALIFORNIA, AND THE RECORDS OF THE COUNTY OF SAN DIEGO, CALIFORNIA.

ZONING INFORMATION

GENERAL ZONING INFORMATION
 ZONING DISTRICT: M-1
 ZONING CODE: M-1
 ZONING DISTRICT: M-1
 ZONING CODE: M-1

OVERALL PROJECT DATA

PROJECT NO.	2024-001
DATE	01/15/2024
PROJECT NAME	VETERANS INDUSTRIAL PARK 215
CLIENT	HILLWOOD
DESIGNER	SCHEMATA DESIGN
DATE	01/15/2024

APN NUMBERS

APN 1	024-001-001-000-000-000
APN 2	024-001-001-000-000-000
APN 3	024-001-001-000-000-000

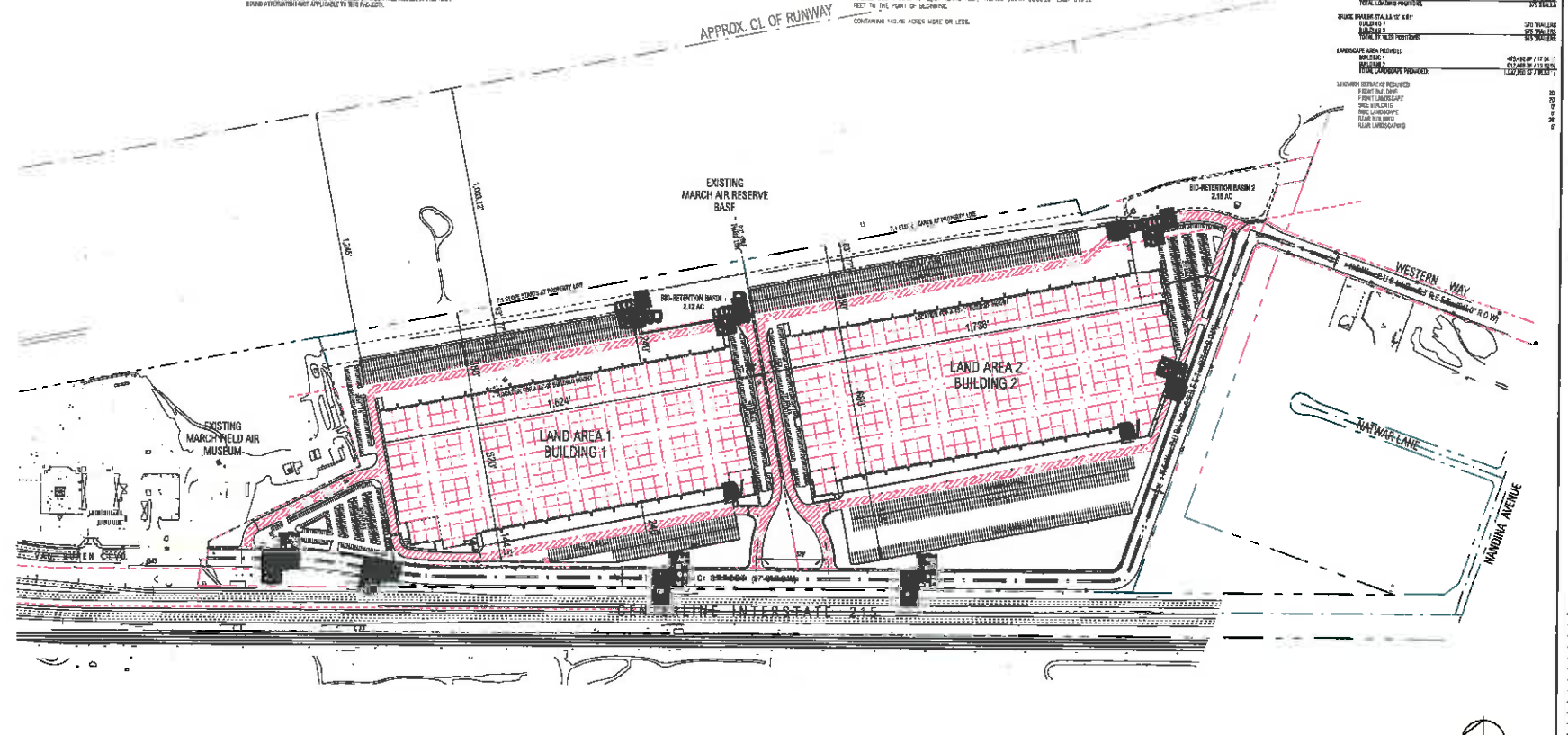
NOTE

1. A LANDSCAPE PROGRAM IS REQUIRED FOR ALL TRACTS.

LAND AREA 1	2.12 AC
LAND AREA 2	2.12 AC
TOTAL	4.24 AC

LANDSCAPE AREA	0.50 AC
TOTAL	4.74 AC

APPROX. CL. OF RUNWAY



OVERALL MASTER SITE PLAN
 SCALE: 1" = 200'-0"



DATE	01/15/2024
PROJECT	VETERANS INDUSTRIAL PARK 215
CLIENT	HILLWOOD
DESIGNER	SCHEMATA DESIGN
DATE	01/15/2024

VETERANS INDUSTRIAL PARK 215
 M.J.P.A. COUNTY OF RIVERSIDE, CA

HILLWOOD
 A FIDELITY+TYSON COMPANY
 RIVERSIDE INLAND DEVELOPMENT, LLC
 501 VIA PIEMONTE, SUITE 175
 ONTARIO, CA 91764
 951-882-0033 PH
 951-882-0078 FAX

NO.	DATE	DESCRIPTION
01	01/15/2024	SCHEMATA DESIGN
02	01/15/2024	SCHEMATA DESIGN
03	01/15/2024	SCHEMATA DESIGN
04	01/15/2024	SCHEMATA DESIGN
05	01/15/2024	SCHEMATA DESIGN
06	01/15/2024	SCHEMATA DESIGN
07	01/15/2024	SCHEMATA DESIGN
08	01/15/2024	SCHEMATA DESIGN
09	01/15/2024	SCHEMATA DESIGN
10	01/15/2024	SCHEMATA DESIGN

OVERALL MASTER PLAN

STANDARD JPA NOTES

- 1. ALL WORK SHALL BE IN ACCORDANCE WITH THE JPA... 2. ALL UTILITIES SHALL BE DEEPENED TO THE SITE... 3. ALL UTILITIES SHALL BE DEEPENED TO THE SITE... 4. ALL UTILITIES SHALL BE DEEPENED TO THE SITE...

LEGAL DESCRIPTION

THE LEGAL DESCRIPTION SHOWN HEREON HAS BEEN PREPARED BY THE SURVEYOR OF RECORDS FOR THE PURPOSE OF RECORDING THE AREA TO BE RECORDED BY THIS SURVEYOR. SAID DESCRIPTION IS NOT TO BE USED FOR CONVEYANCE OF TITLE OF ANY KIND.

ZONING INFORMATION

GENERAL ZONING INFORMATION: THE LEGAL DESCRIPTION SHOWN HEREON HAS BEEN PREPARED BY THE SURVEYOR OF RECORDS FOR THE PURPOSE OF RECORDING THE AREA TO BE RECORDED BY THIS SURVEYOR.

PROJECT DATA - BUILDING 1

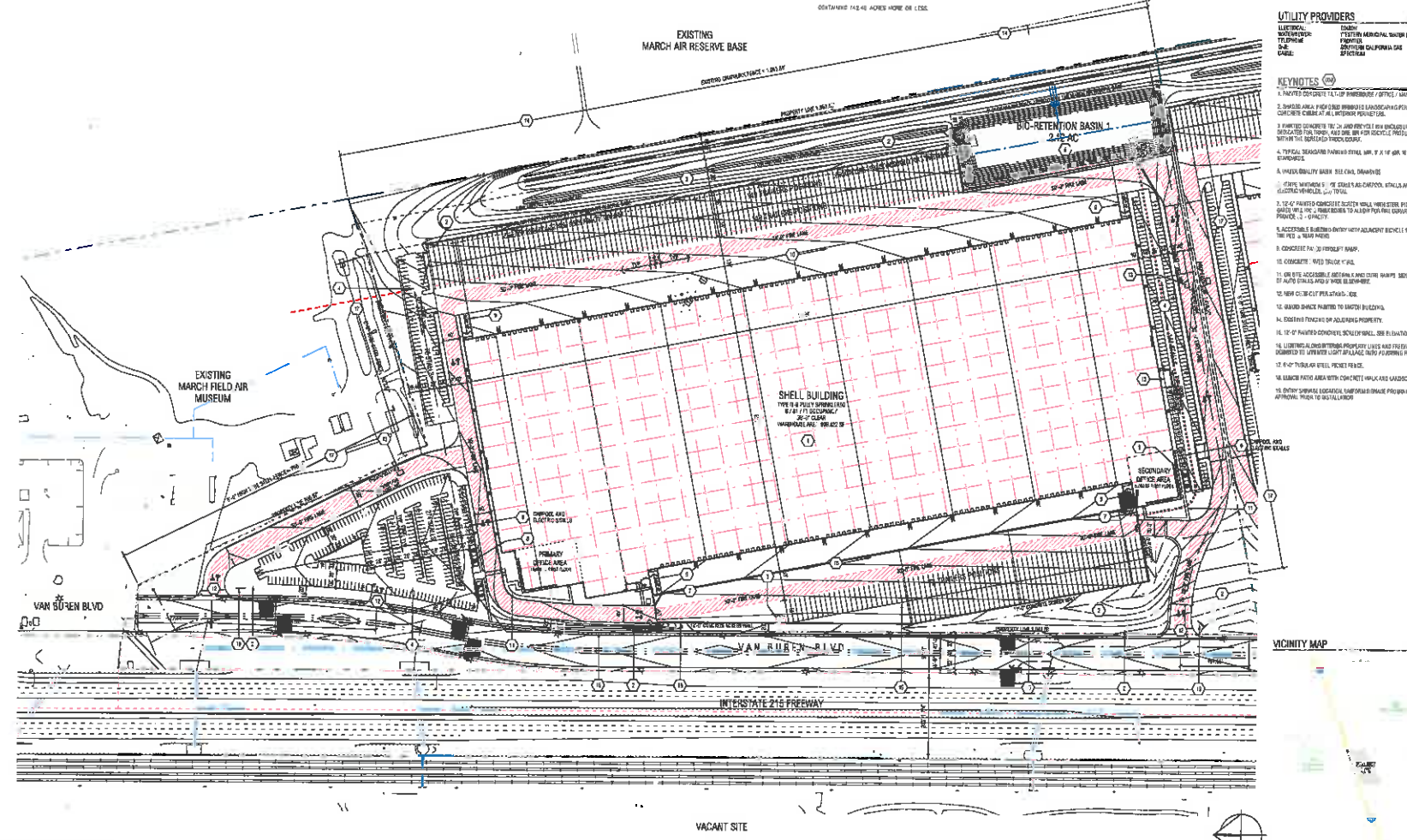
Table with 2 columns: Item, Value. Includes: SITE AREA (256,377 SF), OFFICE AREA (15,000 SF), STORAGE AREA (10,000 SF), etc.

UTILITY PROVIDERS

Table with 2 columns: Utility, Provider. Includes: ELECTRIC (PG&E), WATER (MWD), GAS (SCE), etc.

KEYNOTES

- 1. EXISTING CONCRETE SHALL BE REPAIRED TO MATCH ADJACENT CONCRETE... 2. ALL NEW CONCRETE SHALL BE FINISHED TO MATCH ADJACENT CONCRETE...

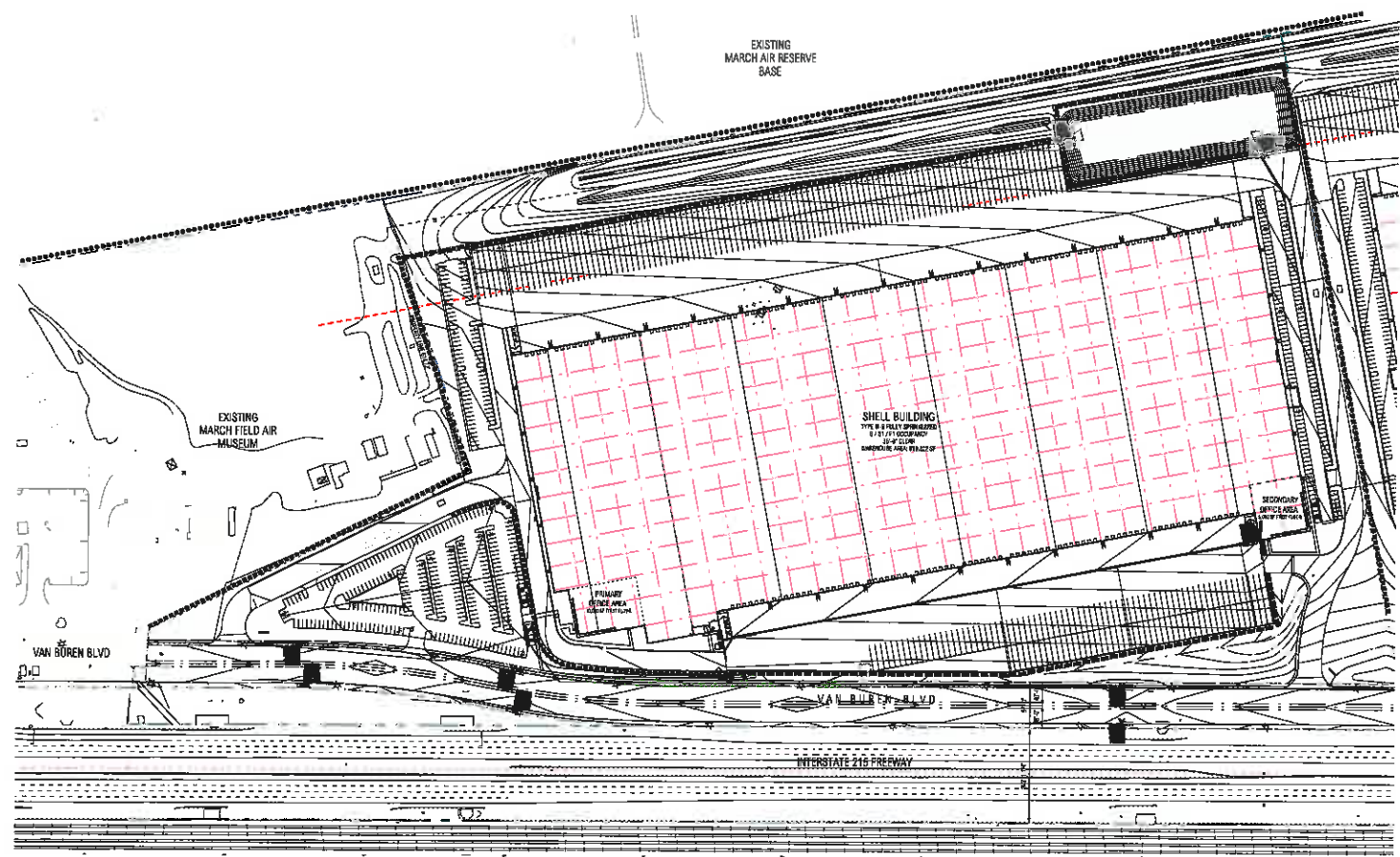


BUILDING 1 SITE PLAN SCALE 1"=100'-0"

Professional seal area for Hillwood, Riverside Inland Development, LLC, including contact information and a table for revision tracking.

SCREENING LEGEND

-----	EXISTING CHAIN LINK FENCE TO REMAIN
-----	NEW 6'-0" (MIN) GALVALUM STEEL 2-STRAND SECURITY FENCE WITH 18" SPACED 1-1/2" X 1-1/2" SQUARE WIRE MESH TO BE INSTALLED TO THE EXISTING CHAIN LINK FENCE
-----	12" W/ BLACK PAINTED TURNBUCKLE STEEL RIVET PILE
-----	12'-0" PAINTED CONCRETE 6-INCH WALL POST AND REVEAL



BUILDING 1 FENCE PLAN
 SCALE: 1" = 100'-0"

PROFESSIONAL SEAL

VETERANS INDUSTRIAL PARK 215
 M/JPA
 COUNTY OF RIVERSIDE, CA

HILLWOOD
 A PEROT COMPANY

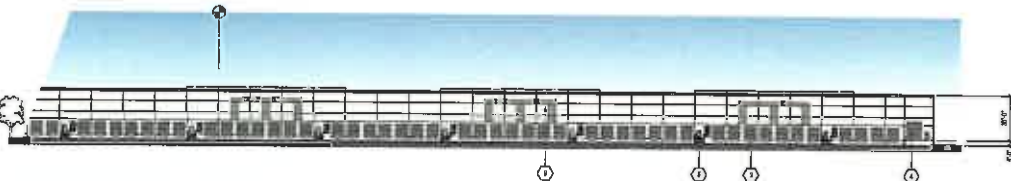
RIVERSIDE ISLAND DEVELOPMENT, LLC
 801 VIA PERMONTE, SUITE 175
 DORTMUND, CA 92504
 951-502-0005 PH
 951-502-0073 FAX

NO.	REVISION	DATE	DESCRIPTION

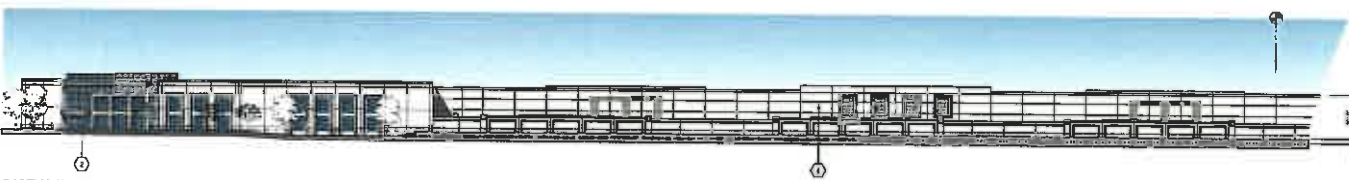
DESIGN PROJECT NO.	11195-00
DESIGN PROJECT DATE	06/01/10
CAD FILE NAME	11195-00-01-2P
DRAWN BY	MS
CHECKED BY	DE
COMPILED BY	DE
DATE	06/01/10
PROJECT NO.	11195-00
PROJECT NAME	VETERANS INDUSTRIAL PARK 215
SHEET TITLE	BUILDING 1 FENCE PLAN



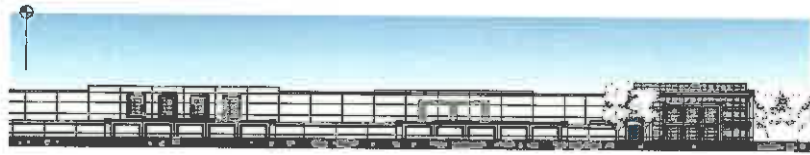
PARTIAL EAST SCREEN-WALL ELEVATION
SCALE 1/4"=1'-0"



PARTIAL EAST SCREEN-WALL ELEVATION
SCALE 1/4"=1'-0"



PARTIAL WEST SCREEN-WALL ELEVATION
SCALE 1/4"=1'-0"



PARTIAL WEST SCREEN-WALL ELEVATION
SCALE 1/4"=1'-0"



OVERALL WEST SCREEN-WALL ELEVATION
SCALE 1/4"=1'-0"

- KEYNOTES** K1
- 1 PRIMARY ACCESSIBLE BUILDING ENTRY
 - 2 BLUE SLABBLE IN CLEAR ANODIZED ALUMINUM SCREEN FRAME
 - 3 PAINTED DOOR WITH TRACKING DOOR
 - 4 PAINTED GRATE (12x12) WITH ACCESS DOOR DOOR
 - 5 2' x 4' PAINTED METAL WALK DOOR
 - 6 PAINTED CONCRETE TILT-UP PANELS WITH REVEAL SCOTES
 - 7 200# STEEL BRACKET SUPPORTS WITH ANODIZED ALUMINUM BRACKET PLATE AND PAINTED DOOR LATCH FROM VIEW OF REAR ELEVATION SCREEN WALL - SEE ELEVATION
 - 8 PAINTED CONCRETE TILT-UP SCREEN WALL WITH REVEAL SCOTES

- FINISH SCHEDULE** FS1
- 1 FLOOR COLOR - 103 TRENKLEWOOD MOUNTAIN
 - 2 ACCENT COLOR - 103 FOGGY GREY ALUM
 - 3 ACCENT COLOR - 103 LILA OBERLINE ALUM
 - 4 ACCENT COLOR - 103 QUINCYVILLE STEEL ALUM
 - 5 SLABBLE - SEE KEYNOTE 2 - PPG SOLARCOLOR PACIFIC CA COLLECTION #2



CONTRACT

PROFESSIONAL SEAL

VETERANS INDUSTRIAL
PARK 215
M/JPA,
COUNTY OF RIVERSIDE, CA

HILLWOOD
A PERINI COMPANY
RIVERSIDE INLAND
DEVELOPMENT, LLC
501 VIA PIEMONTE, SUITE 175
OTISDALE, CA 91764
951-382-6000 PH
951-382-6073 FAX

NO.	
REV.	
DATE	
BY	
CHECKED BY	
DATE	
DESCRIPTION	
ISSUED FOR	BY (DATE)
CLIENT PROJECT NO.	0400000
CAD FILE NAME	144110-100-0
DRAWN BY	MG
CHECKED BY	DR
DATE	08/14/2014
DESCRIPTION	CONCEPTUAL ELEVATIONS
PROJECT	RGA OFFICE OF ARCHITECTURAL DESIGN
SHEET TITLE	CONCEPTUAL ELEVATIONS

Veterans Industrial Park 215

SPECIFIC PLAN

July 2018

Draft

Veterans Industrial Park 215 Specific Plan (SP 16-02)

July 2018

Draft



Prepared For:

March JPA

Applicant:

Riverside Inland Development, LLC

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1 INTRODUCTION

This section explains the purpose of the specific plan; local and regional context and setting; background; planning process and entitlements; authority to prepare; relationship to existing plans and policies; and organization of the specific plan.

1.1 LOCATION AND ACCESS

The Veterans Industrial Park 215 Specific Plan area encompasses approximately 142.5 acres of airport property consisting of Parcel D2 within the boundaries of the March Inland Port Airport, located in Riverside County, California. It is located in the East March Planning Subarea. Parcel D2 is located directly east of the I-215 off-ramp at Van Buren Boulevard, south of the existing March Field Air Museum and west of an existing airport runway; but, provides no access to the runway or any taxiways (flying facilities).

Access to Parcel D2 is provided via the I-215 freeway and Van Buren Boulevard. A southern extension of Van Buren Boulevard is anticipated as a part of this Specific Plan. Figure 1-1, *Location*, shows the regional location of the Specific Plan area.

1.2 BACKGROUND AND HISTORY

March Air Force Base (MAFB) was first established as a military installation in 1918 and has been in near continuous operation between 1918 and 1993. In 1993 the federal government called for the realignment of MAFB and a substantial reduction in its military use. In April 1996, March Air Force Base was re-designated as an Air Reserve Base (ARB). The conversion of MAFB to an air reserve base resulted in the need to dispose of and reuse approximately 4,400 acres of land. In order to limit the economic disruption caused by base closures, the California State Legislature authorized the formation of joint powers authorities to regulate the redevelopment of closed/realigned military installations. The cities of Moreno Valley, Perris, the City of Riverside, and the County of Riverside formed the March Joint Powers Authority (MJPA) pursuant to Article 1, Chapter 5, Division 7, Title 1 (commencing with Section 6500 et seq.). The March JPA was delegated the authority to manage the use, reuse, and joint use of the realigned base.

Since 1996, the MJPA has prepared a number of planning, policy and regulatory documents to guide the redevelopment of the former MAFB. These documents, that impact the Specific Plan area include:

- Final Environmental Impact Statement: Disposal of Portions of March Air Force Base (February 1996)
- Final Environmental Impact Report for the March Air Force Base Redevelopment Project (June 1996)
- Department of Defense Instructions
- Air Force Instructions
- General Plan of the March Joint Powers Authority (September 1999)
- March Joint Powers Authority Development Code (July 1997)
- Master Environmental Impact Report for the General Plan of the March Joint Powers Authority (September 1999)
- Air Installation Compatible Use Zone Study for March Air Reserve Base (2018)
- March Air Reserve Base/Inland Port Airport Joint Land Use Study (December 2010)
- March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan (November 2014)
- Airport Layout Plan (September 2013)

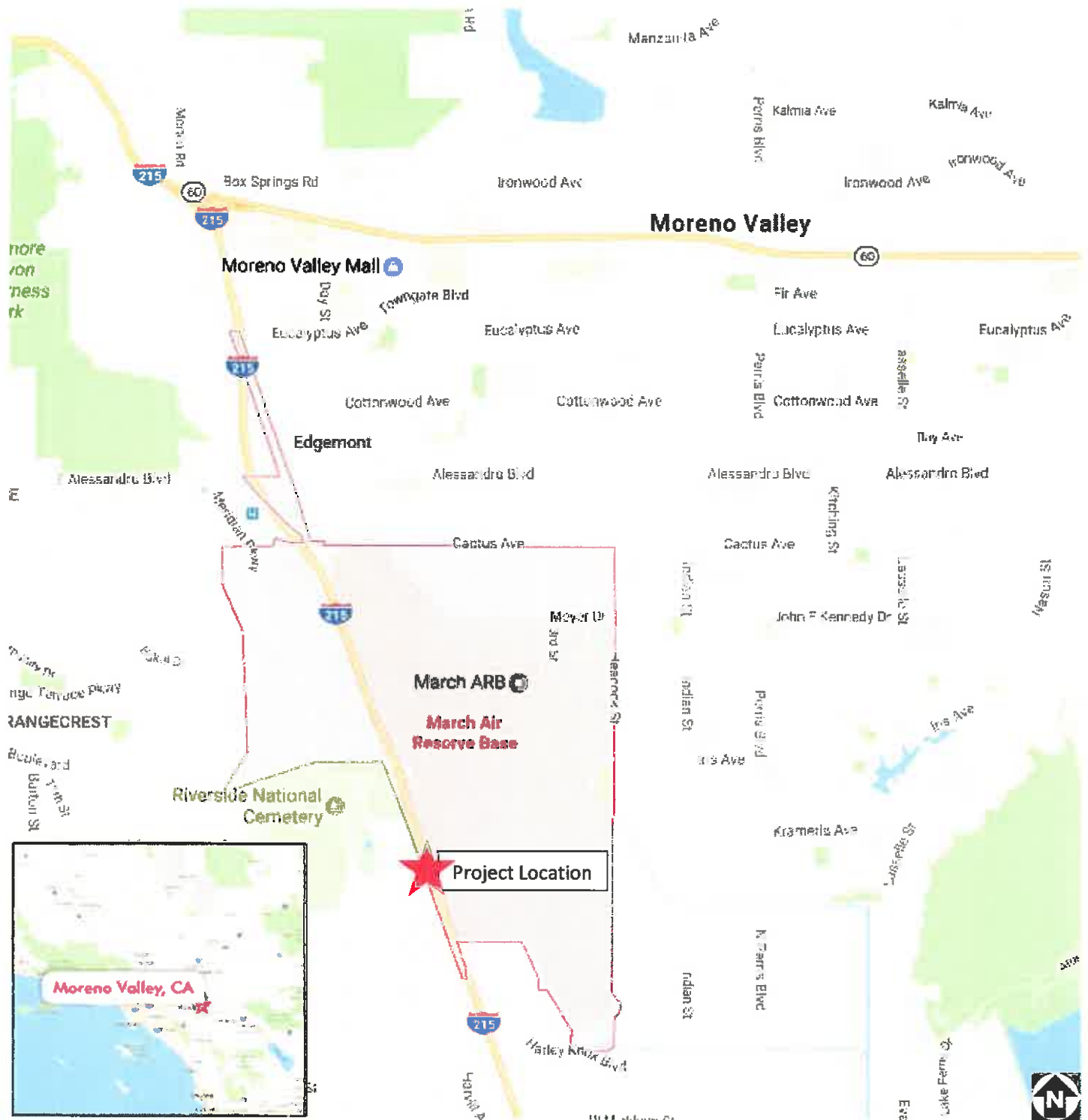


Figure 1-1 Regional Location

In addition to these documents related to development within the airport area, the Perris Valley Commerce Center Specific Plan (PVCCSP) was adopted by the City of Perris in January 2012. The PVCCSP consists of approximately 3,500 acres located immediately south of the Veterans Industrial Park 215 Specific Plan area. Western Avenue, an arterial roadway within the PVCCSP, will be extended to the southern edge of the Specific Plan area as a secondary point of access.

Riverside Inland Development, LLC is the Master Developer for the remaining MIPA owned properties at the March Inland Port. Hillwood entered into an Exclusive Negotiating Agreement (ENA) in December 2015 in response to a Request for Proposal and subsequently entered into a Memorandum of Agreement. The development program addressed by this Specific Plan is the first implementing step of these agreements.

1.3 CONTEXT AND LOCAL SETTING

1.3.1 Specific Plan Area and Ownership

The Specific Plan area is comprised of approximately 142.5 gross acres of vacant land. The project site is composed of five tax parcels, Assessor Parcel Numbers (APNs) 294-150-009, 294-170-005, 295-300-008, 294-140-13 and 294-180-038. The property is presently owned by the March Joint Powers Authority. Figure 1-2 illustrates the Specific Plan area.

1.3.2 Existing General Plan and Zoning

The Specific Plan area consists of approximately 142.5 acres of land that is not currently zoned. Its General Plan designation is Aviation, with an allowable Floor Area Ratio (FAR) of 0.4. This property is expected to be developed under a long-term ground lease.

As part of project entitlements, the property would be subject to a General Plan Amendment that would add a Specific Plan overlay (“SP”) to the existing Aviation designation. In addition, the Veterans Industrial Park 215 Specific Plan would be adopted as the zoning for the property. An analysis of compliance with General Plan goals and policies may be found in the Appendix of this Specific Plan document.

1.3.3 Existing and Surrounding Uses

The property is presently vacant. The 142.5-acre Specific Plan area is surrounded by the following uses:

- North:** Immediately to the north of the development parcel is the existing March Field Air Museum, zoned for Public Facilities uses.
- East:** Property to the east consists of the existing runways of March Air Reserve Base.
- South:** Immediately south of the parcel is the corporate boundary of the City of Perris, and the Perris Valley Commerce Center Specific Plan, zoned for Business Park and Light Industrial uses.
- West:** West of the parcel is I-215, with the Riverside National Cemetery and Meridian Specific Plan area beyond.



Figure 1-2 Specific Plan Area

1.3.4 Existing Infrastructure

Existing Roadways

There is currently no improved roadway access to the Specific Plan area.

Regional access to the Specific Plan area is provided by Interstate 215 (I-215). Interstate 215 runs north/south and is immediately to the west of the Specific Plan area. The nearest freeway ramps are approximately ¼ mile north of the Specific Plan area at Van Buren Boulevard.

Van Buren Boulevard is a primary east/west corridor. It provides access to I-215, and is currently designated as an Arterial Highway to the west of I-215. On the east side of I-215 Van Buren Boulevard is currently planned as a Major Arterial with two northbound lanes, two southbound lanes and a raised landscaped median. It is constructed consistent with this standard for approximately 1,400 feet along the frontage of the March Field Air Museum, although the street's median is painted rather than raised. Van Buren Boulevard currently terminates approximately 300 feet to the north of the Specific Plan area.

Existing Infrastructure

- **Water.** The Specific Plan Area is located within the Riverside retail service area of the Western Municipal Water District (WMWD). Existing Eastern Municipal Water District (EMWD) 8-inch water lines are present in Nandina Avenue and Western Way and a 36-inch water line is located in Harley Knox to the south of the Specific Plan Area in the City of Perris. In addition, an existing 12-inch WMWD water line is present near the northern edge of the Specific Plan Area, within the March Air Museum property.

The U. S. Air Force maintains an existing 354-foot deep groundwater monitoring well, located within the property near the northern boundary with the Air Museum.

- **Sewer Service.** Sewer service in the Specific Plan area is provided by Western Municipal Water District (WMWD). Existing EMWD 10- and 12-inch sewer lines are present to the south of the project in Nandina Avenue in the City of Perris however these lines will not be utilized by the project. In addition, a 10-inch WMWD sewer force main crosses the site's southern edge, continuing west across the I-215 freeway.
- **Drainage.** An existing earthen channel runs from the site's northwest corner to its southeast corner, and a second drainage course is present which runs in an east-west direction, connecting with the earthen channel. This channel collects off-site storm water from four culverts beneath I-215 and conveys it south through the airport property. This earthen channel conveys runoff southerly towards Heacock Street and discharges into Perris Valley Channel in the City of Perris, ultimately discharging to the San Jacinto River, Canyon Lake, and Lake Elsinore.

1.3.5 Airport Constraints

The March JPA has full land use authority over portions of the former base under its direct control. In order to address airport land use compatibility issues around the March Air Reserve Base/Inland Port the Riverside County Airport Land Use Commission prepared and adopted a Land Use Compatibility Plan (March ARB/IPA ALUCP) which serves as the JPA's land use compatibility planning recommendations. In

addition, the Department of Defense Instructions (DDI) and the Air Force Instructions (AFI) apply. The runway system and military areas of the airport are under the control of the U.S. Air Force. There are two active runways at March ARB/IP, Runway 14-32, and Runway 12-30. Runway 14-32, the airport's primary runway, is 13,300 feet long and 200 feet wide. Runway 14-32 has a standard left traffic pattern which means all turns in the traffic pattern departing north are made to the west. The primary runway (Runway 14-32) is located immediately east of the D2 parcel and the Specific Plan area.

The Land Use Compatibility Plan (RCALUP) establishes a boundary for the influence area of March ARB/IPA, related to noise, overflight, safety, and airspace protection. The Specific Plan Area:

- Falls between the 65 and 75 CNEL noise contours of the airport.
- Does not fall within any of the accident zones of the JLUS (Clear Zone or Accident Potential Zones I or II).

A number of regulations and land use/height restrictions affect the Specific Plan area. The property is located within the 7:1 Transitional Surface area of the March Air Reserve Base, and a 35-foot building height limitation line extends along the runway edge as identified on the Airport Layout Plan.

1.3.6 Legal Context

A "Specific Plan" is a planning and regulatory tool made available to local governments by the State of California. Specific plans implement an agency's General Plan through the development of policies, programs, and regulations that provide an intermediate level of detail between General Plans and individual development projects. State law stipulates that specific plans can only be adopted or amended if they are consistent with an adopted General Plan.

The Veterans Industrial Park 215 Specific Plan implements the goals and policies of the General Plan, serves as an extension of the General Plan, and can be used as both a policy and a regulatory document. The purpose of this Specific Plan is to implement the vision by providing goals, policies, programs, development standards, and design guidelines to direct future development within the Specific Plan Area.

The authority to prepare and adopt a Specific Plan and the requirements for its contents are set forth in California Government Code Sections 65450 through 65457. Section 65451 states:

A Specific Plan shall include a text and a diagram or diagrams which specify all of the following in detail:

- *The distribution, location and intent of the uses, including open space, within the area covered by the plan.*
- *The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described by the plan.*
- *Standards and criteria by which the development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable.*

- *A program of implementation measures including programs, public works projects, and financing measures.*
- *The Specific Plan shall include a statement of the relationship of the Specific Plan to the General Plan.*

1.4 DISCRETIONARY ACTIONS

The following discretionary actions will be required in conjunction with the proposed Veterans Industrial Park 215 Specific Plan:

California Environmental Quality Act. This Specific Plan is considered a Project under the California Environmental Quality Act (CEQA). CEQA is a statute that requires state and local agencies to identify the significant environmental impacts of their actions and to avoid or mitigate those impacts, if feasible. To document the potential significant impacts an Environmental Impact Report (EIR) will be prepared for this Specific Plan and must be certified by the Commission prior to adoption of this Specific Plan or any other project entitlements. Subsequent development within the Veterans Industrial Park 215 Specific Plan boundaries deemed consistent with said Specific Plan standards will not require further environmental review except as specified in the Development Regulations section of this document (Section 3). March JPA will be the lead agency responsible for certification of the Project's EIR.

General Plan Amendment. A General Plan Amendment to attach a Specific Plan overlay to the Specific Plan Area. The General Plan Amendment will be adopted by resolution. The existing General Plan land use designation of Aviation would be expanded to include general warehousing and logistic uses.

Specific Plan/Zone Change. The Specific Plan Area is presently un-zoned. The establishment of the Specific Plan for the proposed project will provide for an SP-8 designation on the March JPA Zoning Map with an underlying Aviation Designation. Adoption of this Specific Plan as part of a zone change is a discretionary action subject to March JPA approval. Adopted by Ordinance, the Specific Plan document will serve both planning and regulatory functions. This document contains the development standards and procedures necessary to fulfill these purposes.

Tentative Parcel Map. The Specific Plan Area is comprised of five assessor parcels for taxation purposes but is not presently a legal development parcel. The proposed project will include a Tentative Parcel Map to create two legal development parcels, dedicate rights-of-way for the extension of Van Buren and Western Avenues and identify required utility easements.

Plot Plan. The site development plan for the project, consisting of two buildings of approximately 2,185,618 square feet, parking, landscaping, drainage facilities, and new streets.

Development Agreement (DA) and Disposition and Development Agreement (DDA). A statutory development agreement, authorized pursuant to California Government Code Section 65864 et seq., will be processed as part of the approval of this Specific Plan. The development agreement of this Specific Plan will include, among other items, methods for financing acquisition and construction of infrastructure,

and phasing, including future phasing. Such development agreement shall be fully approved before the issuance of the first building permit for this project.

Avigation Easement. Development projects must provide an executed easement to the MIPA prior to a final map or building permit.

Requested Permit/Approval	Approving Agency
Final EIR Certification	March JPA
Specific Plan Adoption	March JPA
General Plan Amendment	March JPA
Development Agreement	March JPA
Tentative Parcel Map Approval	March JPA
Water Supply Assessment	Western Municipal Water District
Encroachment Permit (Drainage)	Caltrans
Encroachment Permit (Western Way)	City of Perris
1602 Permit	California Department of Fish and Game
404 Permit and associated EA	U.S. Army Corps of Engineers
401 Permit	Regional Water Quality Control Board
NPDES	Regional Water Quality Control Board

1.5 PLAN ORGANIZATION

Section 1 – Introduction

This section explains the purpose of the Specific Plan; local and regional context and setting; background; planning process and entitlements; guiding principles; authority to prepare; relationship to existing plans and policies; and organization of the Specific Plan.

Section 2 – Development Plan

This section explains the conceptual land use plan for the Specific Plan Area; identifies land use policies, and defines the land use designations unique to the Specific Plan. The circulation, drainage, water and sewer, grading, and public services plans are also described.

Section 3 – Development Regulations

This section explains the development standards for the land use designations established in the Development Plan, including the standards for allowable uses, setbacks, parking, and signage.

Section 4 – Design Guidelines

This section explains design concepts and establishes design guidelines for development in the Specific Plan Area.

Section 5 – Administration and Implementation

This section discusses the development review procedures by the JPA and other relevant permitting agencies, applicable to the Specific Plan Area. Implementation of the proposed land uses, including Specific Plan adoption, subsequent approvals and plans, substantial conformance findings, and phasing are outlined in this chapter. Additionally, financing sources and maintenance responsibilities are identified.

2 DEVELOPMENT PLAN

This chapter explains the various elements of the Veterans Industrial Park 215 Specific Plan, including Land Use, Circulation, Public Services, and Infrastructure.

2.1 LAND USE

This section of the Veterans Industrial Park 215 Specific Plan discusses the components of the Specific Plan such as land use, circulation, grading, drainage, water and sewer utilities, and public services.

Each of these components is discussed in further detail in the sections below.

2.1.1 Project Objectives

The proposed Veterans Industrial Park 215 Specific Plan is intended to achieve the following objectives:

- Develop and operate a state-of-the-art logistics center that takes advantage of existing and planned March Joint Powers Authority (“MJPA”) infrastructure, is feasible to construct, and is economically competitive with, and in the general vicinity of, similar industrial, logistics and distribution center uses.
- Develop and operate a large format logistics center that is in close proximity to the former March Air Reserve Base and I-215/State Route 60 to support the distribution of goods throughout the region and that also limits truck traffic disruption to sensitive receptors within the surrounding region.
- Develop and operate a large format logistics center that may accommodate e-commerce opportunities, attract quality tenants and will be competitive with other similar facilities in the region.
- Maximize efficient goods movement throughout the region by locating a large format logistics center in close proximity to the Ports of Los Angeles and Long Beach thereby enabling trucks servicing the site to achieve a minimum of two roundtrips per day.
- Develop and operate a large format logistics center that maximizes the use of a large industrial site in the region that is in close proximity to the Ports of Los Angeles and Long Beach, to realize substantial unmet demand in the region, allowing the region to compete on a domestic and international scale through the efficient and cost-effective movement of goods.
- Develop and operate a large format logistics center that meets and/or exceeds industry standards for operational criteria, including energy efficiency.
- Implement the Veterans Industrial Park 215 Specific Plan through development of an airport land use program that is consistent with the proposed development standards and criteria relevant to the site and proposed large format logistics center use.
- Facilitate the development of underutilized land currently planned for aviation-related uses that maximizes the use of the site and responds to market demand within the Veterans Industrial Park 215 Specific Plan area and surrounding region for a large format logistics center.
- Facilitate the establishment of design guidelines and development standards consistent with the MJPA Development Code and that create a unique, well-defined identity for the Veterans Industrial Park 215 Specific Plan.
- Positively contribute to the economy of the region through new capital investment, creation of new employment opportunities, including opportunities for highly-trained workers and replacement jobs for those lost due to military base closures, and expansion of the tax base.

- Provide for off-site realignment of existing drainage channels in order to minimize potential drainage and related impacts associated with the proposed large format logistics center, including impacts to aviation uses at the adjacent runway.
- Provide for the extension of planned roadways consistent with the MJPA Circulation Element.
- Identify and provide for water, sewer, drainage, and road facility infrastructure that is required to adequately service the Veterans Industrial Park 215 Specific Plan area for the intended uses.
- Establish landscape guidelines that emphasize the use of drought-tolerant and water-efficient plant materials.
- Establish guidelines for energy efficiency that promote the conservation of energy resources in the construction and operation of the proposed large format logistics center use.

2.1.2 Land Use Plan

The Veterans Industrial Park 215 Specific Plan envisions two logistics structures totaling 2,185,618 square feet, with loading docks, truck trailer parking, and associated infrastructure improvements. The primary logistics use would include typical ancillary uses as outlined in the project's Development Regulations, including warehouse, office, and employee support areas such as meeting rooms and break rooms.

The plan, shown in Figure 2-1 *Conceptual Land Use Plan* and Figure 2-2, *Conceptual Site Plan*, provides the overall vision and guide for the development of the site for logistics uses. Table 2-1, *Land Use*, outlines the maximum build-out for each planning area.

Although the land use plan identifies two planning areas, if a single user is identified who wishes a single building, buildings may be constructed across planning area boundaries so long as the maximum Floor Area Ratio across the entire area does not exceed the maximum Floor Area Ratio (FAR) for the project as a whole. Individual Planning Area square footages may vary as part of plot plan review so long as the FAR for the Specific Plan Area as a whole does not exceed the maximum allowable FAR of 0.4.

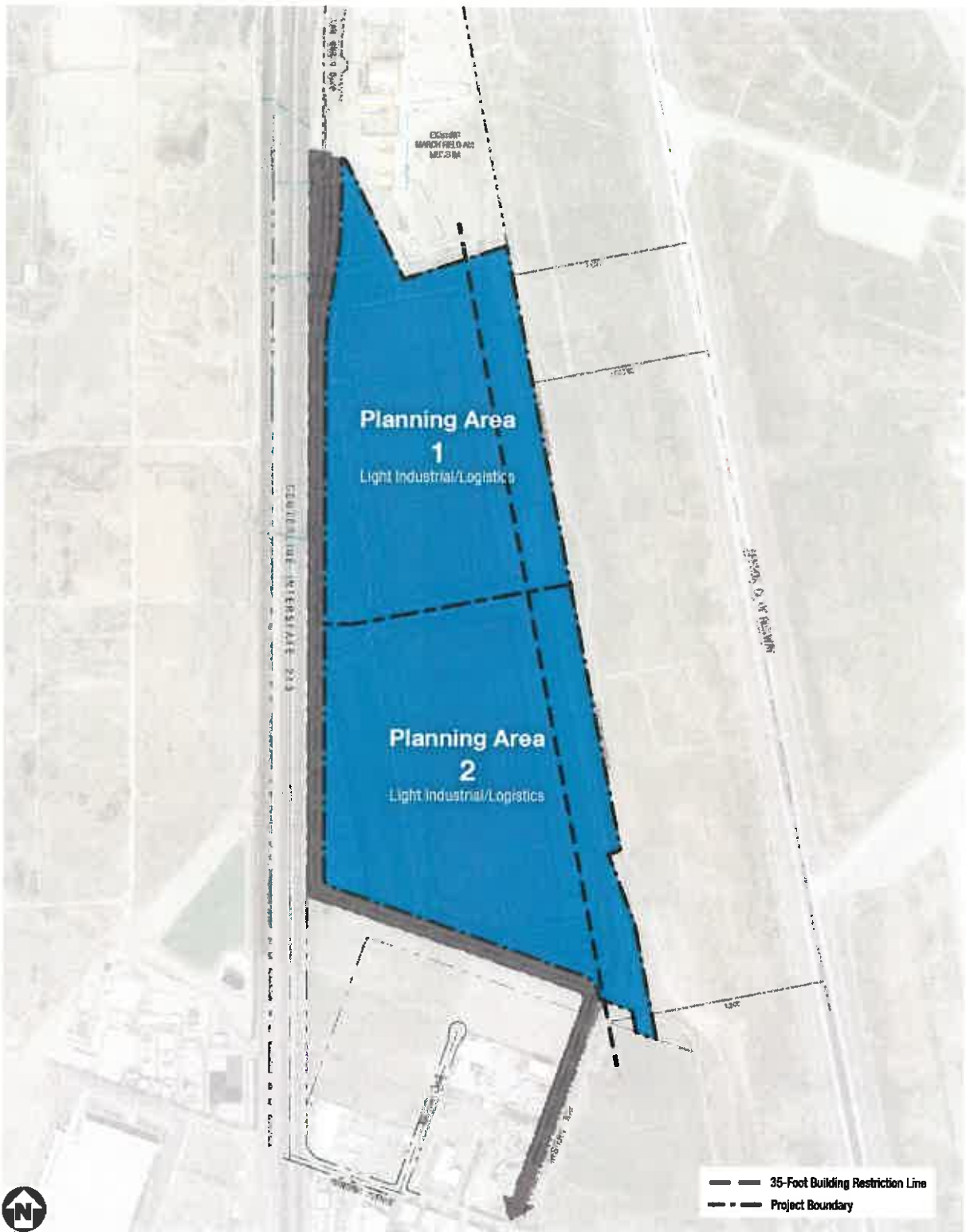


Figure 2-1 Conceptual Land Use

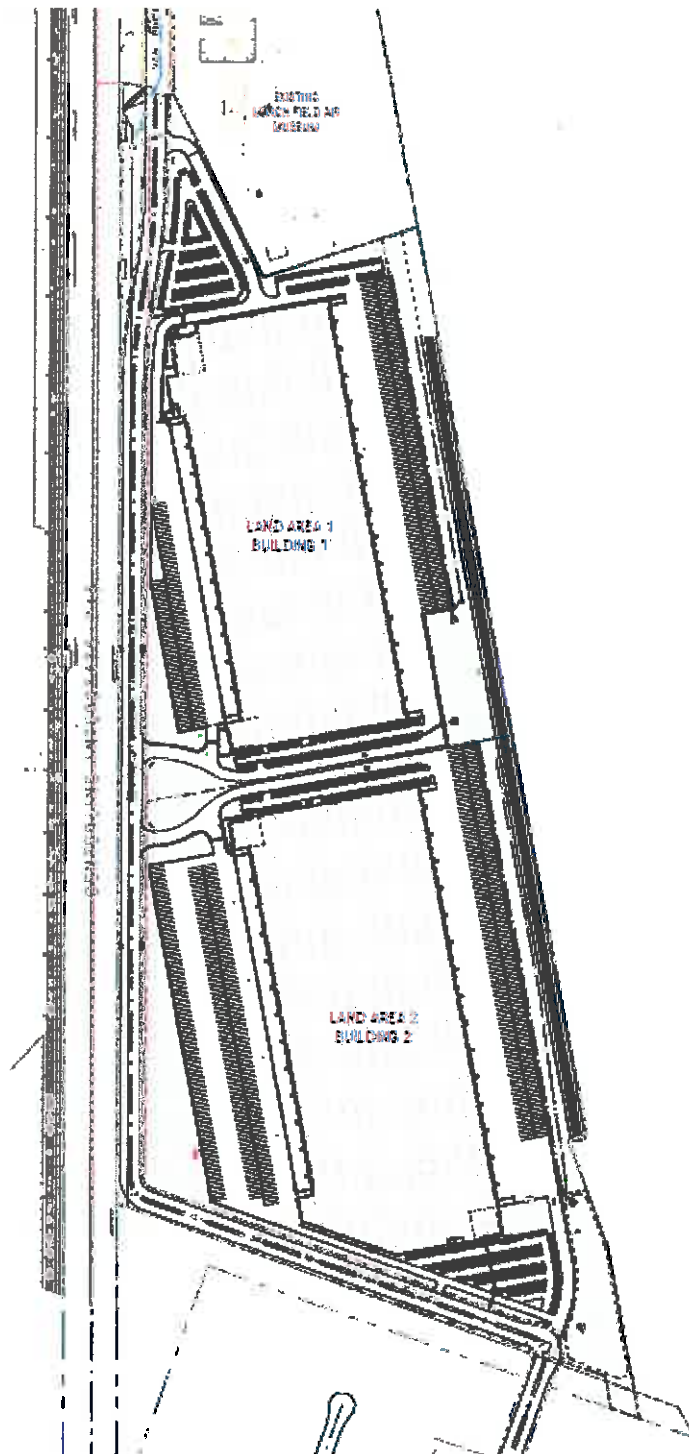


Figure 2-2 Conceptual Site Plan

For purposes of this Specific Plan, if mezzanines are provided as part of a building, only mezzanines providing occupancy (i.e., second floor enclosed office area) are counted in the square footage identified in the land use table or included in the maximum FAR.

Planning Area	Land Use	Acreage	Building Area (Sq. Ft.)	Floor Area Ratio (FAR)
Planning Area 1/Building 1	Logistics/Light Industrial	57.47 ac.	1,014,822	0.393
Planning Area 2/Building 2	Logistics/Light Industrial	70.38 ac.	1,170,796	
Road Dedication	--	14.61 ac.	--	--
Total		142.46 ac.	2,185,618 (2,227,660 maximum)	0.35 (gross) 0.393 (net) 0.4 (maximum)

2.1.3 Land Use Compatibility

This Specific Plan is subject to the development restrictions of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan (RCALUP), Department of Defense Instructions (DODI) and Air Force Instructions (AFI). The RCALUP includes nine land use compatibility zones. The aeronautical factors used to establish the compatibility zone boundaries are described below and summarized in Exhibit 3-2, Compatibility Zone Factors (see Appendix B). The Compatibility Map (Exhibit 3-3 in Appendix B) depicts the compatibility zones for March ARB and Inland Port Airport (IPA). These compatibility zones and the factors upon which they are based are similar in concept to the compatibility zones adopted by the Riverside County ALUC for other airports in the county. However, the characteristics of aircraft activity at March ARB/IPA compared to primarily general aviation activity at the other airports in the county required the development of zones based upon somewhat different factors.

The site is located within Zone B2. Zone B2 encompasses areas of high noise, but is subject to less risk. The projected 65 decibel noise contour forms the basis for the zone boundary. The actual boundary follows roads, parcel lines or other geographic features that lie generally just beyond the contour line. Lands within the APZs are excluded from Zone B2. Most of the zone lies adjacent to the runway.

Lying just beyond the Air Force defined APZs, the areas within this zone are subject to sufficient risk to warrant restrictions on the intensity of nonresidential development. Specifically, nonresidential uses would be limited to maximums of 100 people per acre average over a site and 250 people in any single acre. These limits are designed to preclude intensive uses such as major shopping centers and large restaurants. Light industrial uses and office buildings up to three stories are typically consistent with the criteria.

The Specific Plan Area's location adjacent to the runway area subjects the site to a number of unique conditions which are reflected in the project's development regulations and site design, including:

- Height limitations in the 7:1 surface and a building restriction line for structures of over 35 feet;
- Security fencing along the runway property and a clear area beyond;
- Limitations on standing water; and
- Limitations on reflective surfaces facing the runway.

2.2 CIRCULATION PLAN

2.2.1 Regional

Regional access to the Specific Plan area is provided by Interstate 215 Freeway (I-215). I-215 runs north/south and is immediately to the west of the Specific Plan area. The nearest freeway ramps are approximately ¼ mile north of the Specific Plan area at Van Buren Boulevard. Interstate 215 is currently constructed with three lanes in each direction. Ultimate buildout for Interstate 215 is 10 lanes; 4 mixed-flow lanes and one high occupancy vehicle lane in each direction. The nearest on-ramps are present at Van Buren Boulevard. Van Buren Boulevard also provides regional connections to State Route 91 and 60 Freeways.

Figure 2-5, *Circulation Plan*, identifies the planned roadway system serving the Specific Plan area.

2.2.2 Arterial Highways

Van Buren Boulevard Extension

Van Buren Boulevard is a primary east/west corridor. It provides access to I-215, and is currently designated as an Augmented Urban Arterial to the west of I-215.

On the east side of I-215, Van Buren Boulevard is currently planned as a Major Arterial with two northbound lanes, two southbound lanes and a raised landscaped median. It is constructed consistent with this standard for approximately 1,400 feet along the frontage of the March Field Air Museum, although the street's median is painted rather than raised. Van Buren Boulevard currently terminates approximately 300 feet to the north of the Specific Plan area.

Project-related improvements would include the extension of Van Buren between its existing terminus and the project site, constructed as a divided Modified Secondary Highway, with two northbound and two southbound lanes with a center turn median design, and an on-street bicycle lane. The Modified Secondary Highway street section would include a 97-foot right-of-way (see Figure 2-3, *Van Buren Extension Typical Cross Section*) along the frontage with I-215 and along the site's southern edge, connecting with Western Way. The proposed Van Buren extension improvements would occur within the property boundaries and would be dedicated to the JPA. The Van Buren Boulevard extension by definition (JPA General Plan) is a designated truck route.

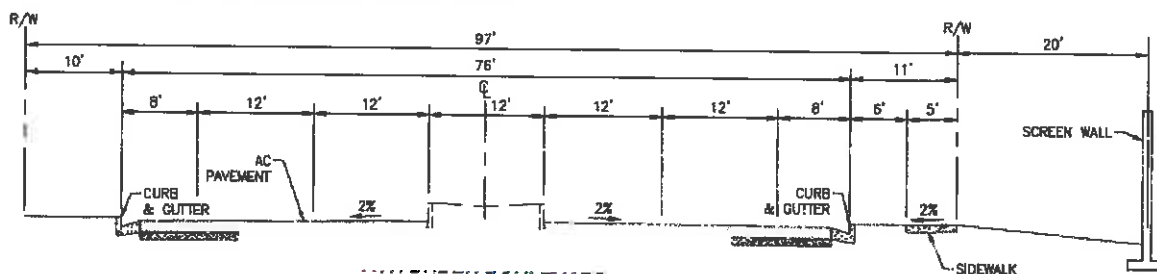


Figure 2-3 Van Buren Extension Typical Cross Section

Western Way Extension

Secondary access to the Specific Plan area would be provided through the construction of a new roadway extending south from the site's southeastern border to Nandina Avenue, as an extension of existing Western Way.

The Western Way extension would be constructed as a Secondary Arterial as defined in the Perris Commerce Center Specific Plan, with a right-of-way of approximately 94 feet (see Figure 2-4, *Western Way Typical Cross Section*). This road would be an off-site infrastructure improvement encompassing approximately three acres. Western Way is a secondary arterial in the City of Perris Commerce Center Specific Plan originally planned to extend to the March JPA boundary. It is a designated truck route.

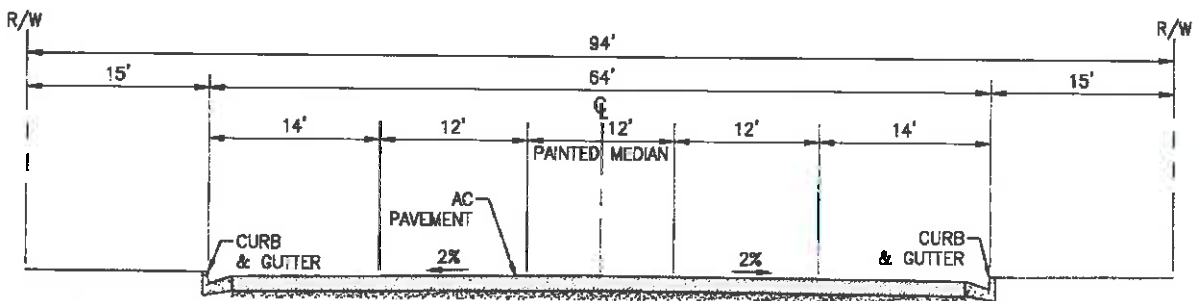


Figure 2-4 Western Way Typical Cross Section

2.2.3 Site Access

The Specific Plan incorporates six driveways to provide direct access from the planned Van Buren Avenue and Western Way extensions. Site access may be controlled by security gates within the interior of the site, subject to review by the Fire Department for stacking and emergency access. Additional site access points may be provided subject to plot plan review for conformance with JPA requirements limiting driveways to one for every 300 feet of frontage.

2.2.4 Parking

Parking within the Specific Plan area will be in several locations. Employee and visitor parking will be provided in parking areas separated from the truck docks and service areas as shown on the Conceptual Site Plan, Figure 2-2. These parking areas will be landscaped, with parking lot trees shading parking spaces. Truck docks and trailer parking stalls are located on the front and rear of each building. Truck docks and parking on the front of the project facing Van Buren Boulevard will be screened with a screen wall. Bicycle parking will be provided. Parking standards for the Specific Plan area are outlined in the Development Regulations section of this document (See Section 3, *Development Regulations*).

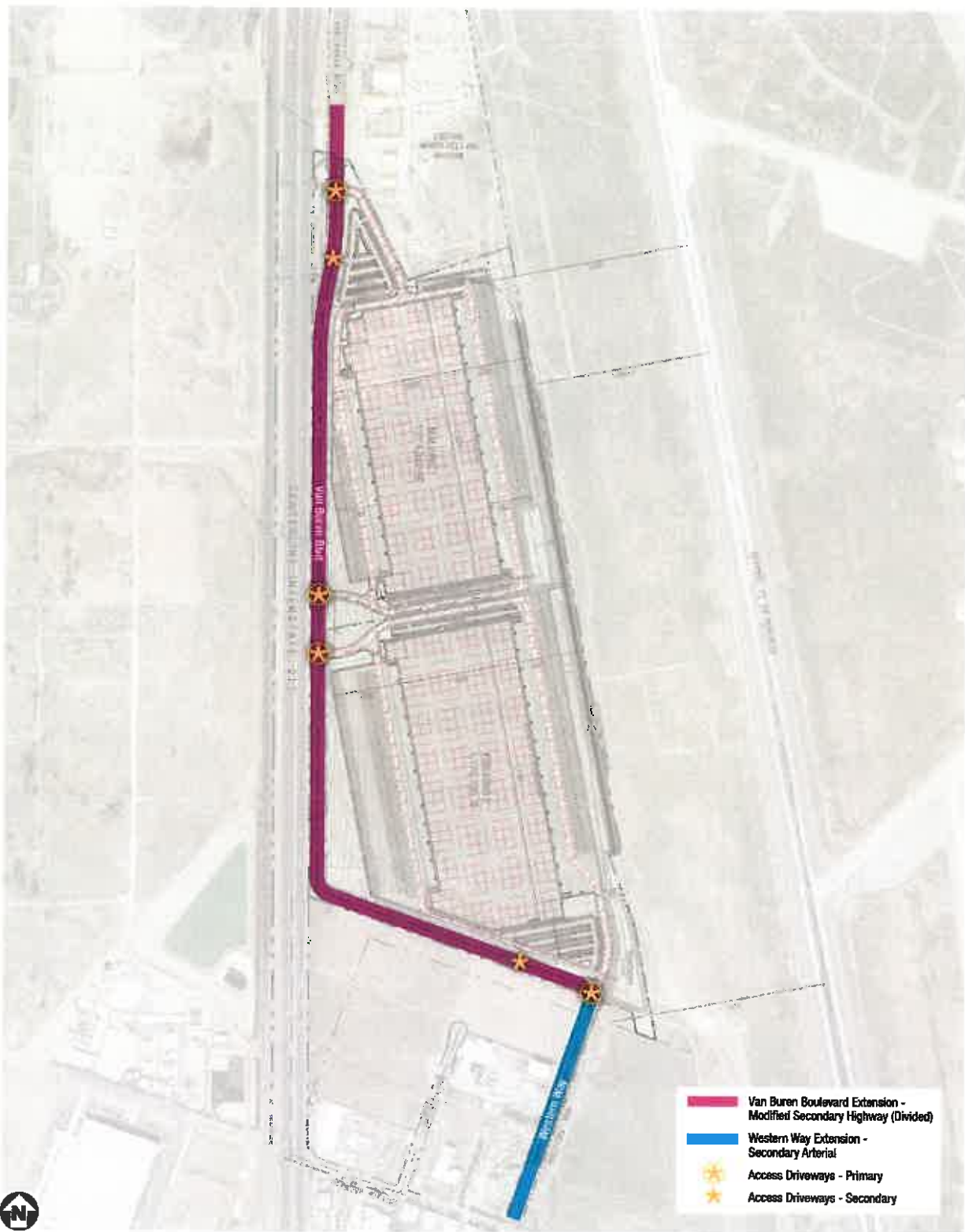


Figure 2-5 Circulation Plan

2.3 INFRASTRUCTURE AND SERVICES

The Veterans Industrial Park 215 Specific Plan will require a variety of public facilities and services to support and serve the needs of its businesses. The infrastructure system will seek to incorporate the highest level of sustainability achievable for a project of its kind and in its specific geographic location.

The various public facilities will be designed to enhance and complement the vision and design objectives of the Project and all facilities will be developed to meet or exceed the required industry standards of the respective service providers and as required by the applicable government standards

Services include: water, sewer, storm drainage, solid waste disposal, fire and police protection services. Table 2-2, *Service Providers*, lists the various service providers for the Project.

Service	Provider
Water	Western Municipal Water District (potable) Eastern Municipal Water District (Fire Flow)
Wastewater	Western Municipal Water District
Drainage	Riverside County Flood Control District
Electric Service	Southern California Edison
Gas Service	Southern California Gas Company
Communications	Frontier/Spectrum
Fire Protection	Riverside County Fire Department
Police Protection	Riverside County Sheriff
Solid Waste Disposal/Recycling	Waste Management Inland Empire

2.3.1 Water Service

The Specific Plan Area is located within the Riverside retail service area of the Western Municipal Water District (WMWD). WMWD is a member agency of the Metropolitan Water District (MWD), purchasing water from MWD and providing wholesale and retail water within its district boundaries. Water sources from WMWD primarily depend on imported water resources. WMWD purchases both Colorado River and State Water Project water from Metropolitan Water District of Southern California (MWD). Fire water will be provided by Eastern Municipal Water District (EMWD) through an Inter-Agency Agreement between EMWD and WMWD. A Plan of Services will be prepared to identify construction of new facilities and required easements.

Existing Facilities

Existing EMWD 8-inch water lines are present in Nandina Avenue and Western Way to the south of the Specific Plan Area in the City of Perris. According to EMWD, fire flow supply is available from EMWD's 1705 pressure zone. In addition, an existing 12-inch WMWD water line is present near the northern edge of the Specific Plan Area, within the March Air Museum property.

MWD maintains a currently unused 97-inch transmission main in an easement that follows the proposed alignment of the Western Way extension and the Van Buren Boulevard extension. This line will be protected in place.

The U. S. Air Force maintains an existing 354-foot deep groundwater monitoring well, located within the property near the northern boundary with the Air Museum. This well will be protected in place within the truck loading area on the runway side of Building 1/Planning Area 1. An easement will be recorded providing for access to the well.

Proposed Facilities

Domestic Water. The proposed domestic water would be provided by WMWD. Water for the project would be extended from the existing 12-inch WMWD water line within the March Air Museum property to Van Buren Boulevard then southwards along the extension of Van Buren Boulevard. Water laterals would extend from the 12-inch lines to service the buildings.

Water for Fire Suppression. Water for fire suppression systems will be provided to the buildings through extension of an 18-inch EMWD line located in Western Way. The 18-inch fire service line will extend northwards within the Van Buren extension right-of-way. A new 12-inch fire water line loop will be provided within the Specific Plan area to each building to provide for on-site fire water supply. Fire flow of 4,000 gpm at 20 PSI is required by the Fire Department.

Due to the size of the proposed industrial/logistics uses, a Water Supply Assessment will be required, provided by WMWD.

Figure 2-6, *Water and Sewer Plan*, illustrates the existing and proposed water infrastructure to serve the site. Changes in water and sewer line size and alignment may occur as part of final engineering.

The Project would be required to plan and install water-efficient devices and landscaping in accordance with applicable ordinances, including use of drought tolerant species appropriate to the climate and region. Although the Project is not located near any existing recycled water distribution infrastructure, should such facilities be constructed in the future, the Project would be required to use non-potable recycled water for irrigation to the extent permitted by law.

2.3.2 Wastewater Service

Sewer service in the Specific Plan area is provided by Western Municipal Water District (WMWD).

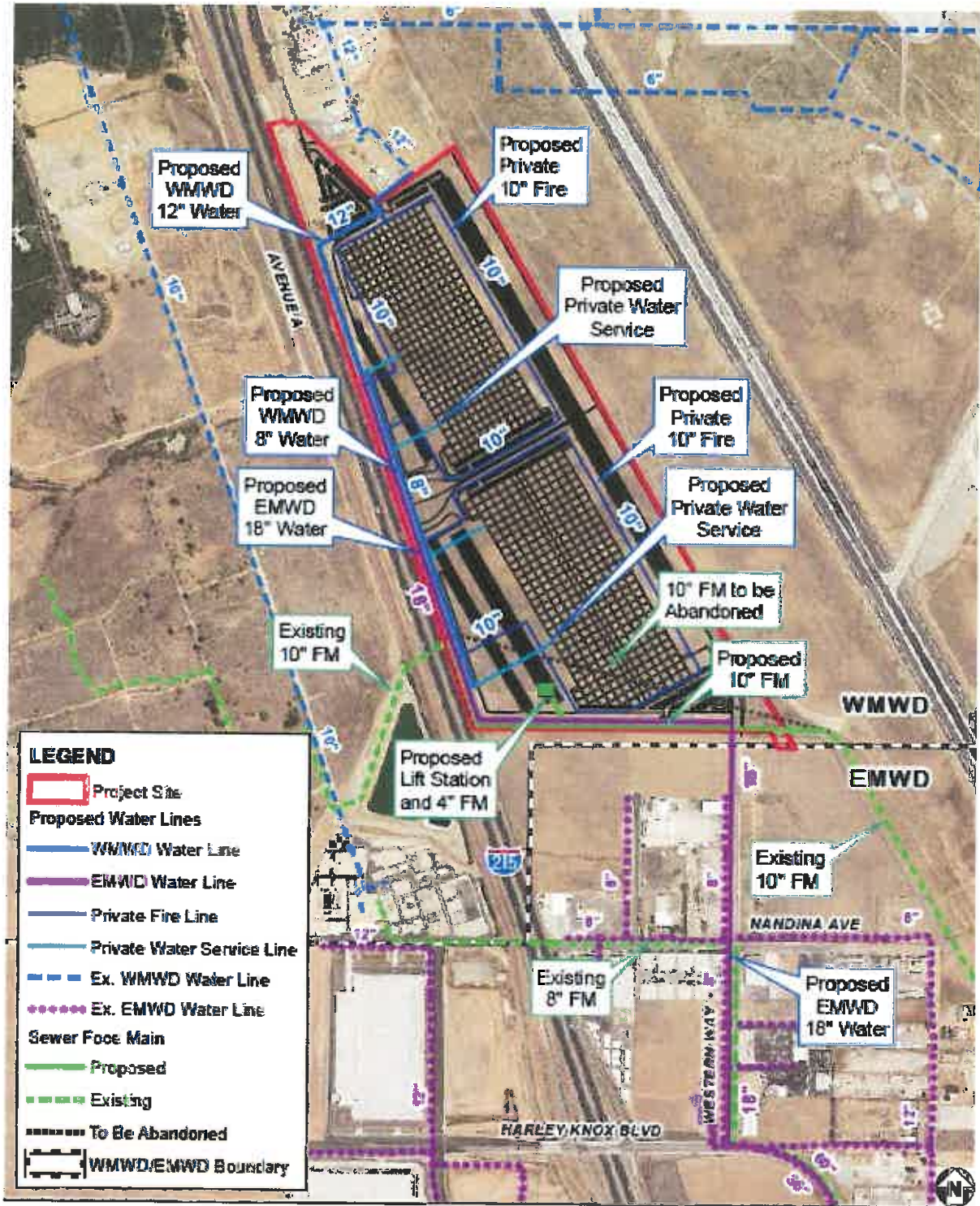
Existing Facilities

Existing 10- and 12-inch sewer lines are present to the south of the project in Nandina Avenue in the City of Perris, however these lines are within EMWD and will not serve the project. In addition, a 10-inch WMWD sewer force main crosses the site's southern edge, continuing west across the I-215 freeway.

Proposed Facilities

The proposed project would relocate the existing WMWD sewer force main within the proposed alignment of Van Buren Boulevard along the Specific Plan area's southern edge.

Wastewater from the Specific Plan area would be conveyed via on-site private sewers to a proposed sewer sump and pump system, connect to a new force main, and then connect with a relocated force main. Figure 2-6, *Water and Sewer Plan*, illustrates the existing and proposed wastewater infrastructure to serve the site. Changes in water and sewer line size and alignment may occur as part of final engineering.



Source: Webb Associates

Figure 2-6 Water and Sewer Plan

2.3.3 Storm Water Management

The project site slopes gently from northwest-to-southeast, with elevations ranging from approximately 1,525 feet to 1,500 feet above mean sea level. An existing earthen channel runs from the site's northwest corner to its southeast corner, and a second drainage course is present which runs in an east-west direction, connecting with the earthen channel. This channel collects off-site storm water from culverts beneath I-215 and conveys it south through the airport property. This earthen channel conveys runoff southerly towards Heacock Street and discharges into Perris Valley Channel in the City of Perris, ultimately discharging to the San Jacinto River, Canyon Lake, and Lake Elsinore. The Specific Plan Area is located in the Riverside County Flood Control District's Perris Valley Area Drainage Plan.

Off-Site Drainage

Off-site drainage from upstream properties to the west of the Specific Plan Area is discharged onto the site and into the existing earthen channels from four Caltrans culverts under I-215. To develop the site, this off-site water will be conveyed directly to a proposed concrete-lined trapezoidal channel on the Specific Plan Area's eastern edge adjacent to the runway, ultimately discharging to the existing earthen drainage swale at the site's southeastern corner. This off-site water will not be comingled with untreated on-site flows.

Project Drainage

The site was divided into two parcels/drainage areas with stand-alone drainage facilities, consistent with the two Specific Plan Planning Areas. As noted above, the existing earthen channel will be realigned to the property's eastern edge, and a concrete trapezoidal channel constructed. On-site storm water will be collected, either by surface flow or storm drains, and directed to two bio-retention/detention basins. Each basin is sized to have storage capacity above the water quality treatment volume to detain higher storm events. A pump system will be used to convey water from the basins to the proposed trapezoidal channel since the bottom of the basin is lower than the proposed channel. These pumps will regulate the rate of discharge to mimic pre-development flow rates.

All drainage facilities are sized for the 100-year storm event. All observable water in both basins will be pumped out within 24 hours of the end of a storm event. This is a betterment from MJPA's standard criteria of 48 hours. Figure 2-8, *Drainage Plan*, illustrates the drainage concept for the project.

Water Quality

The site is subject to water quality requirements of March Joint Powers Authority (MJPA) and complies with the 2010 Santa Ana MS4 permit. These require that Low Impact Development (LID) Best Management practices (BMPs) are limited to *Infiltration or Harvest* and/or bio-retention unless proven infeasible. A WQMP was prepared for the project by Huitt-Zollars (October 2016). The WQMP concludes that both Infiltration and Harvest are infeasible and other treatment control BMPs must be considered. Infiltration was deemed infeasible because the Geotechnical Engineer has determined that the project site soils have no infiltration capacity and recommended that infiltration BMPs should not be used. The soil is impermeable and infiltration rates have been determined to be less than 1.6 inches/hour. Harvest and Use is not utilized because the anticipated demands for irrigation and toilet use are less than their respective required amounts. Therefore, bio-retention BMPs are considered for this site.

Thus, the primary BMPs to be implemented will be construction of two bio-retention/bio-treatment basins (refer to Figure 2-7, *Drainage Plan*). Catch basin filters will be provided in all on-site catch basins as

pre-treatment control prior to water being conveyed to the basins. The water collected into each basin is then pumped to a storm drain channel to the East and ultimately conveyed to the existing earthen channel which flows towards the Perris Valley Channel.

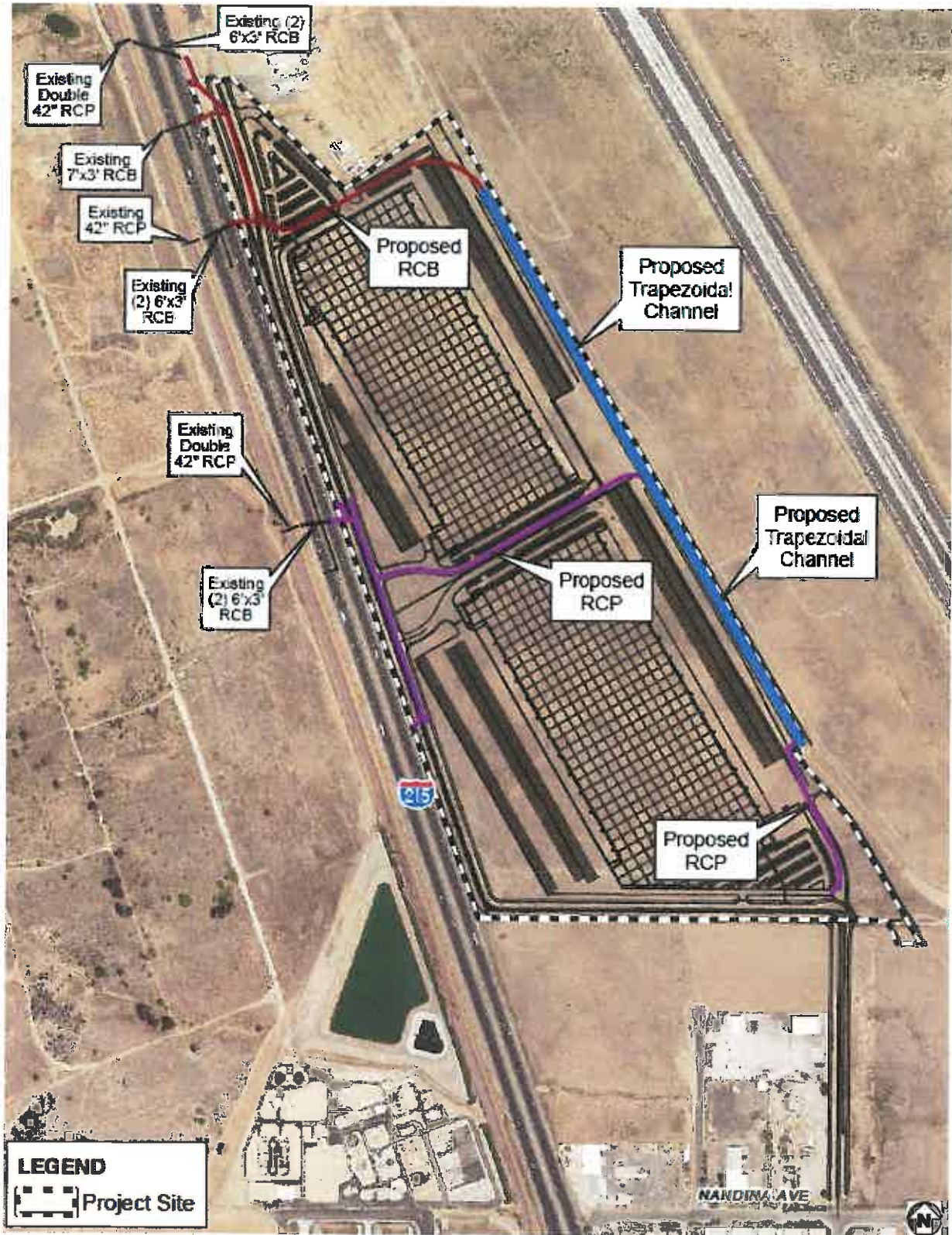
The site is located within an area identified by the Riverside County Flood Control District as being within an area susceptible to streambed erosion (aka Hydrologic Conditions of Concern, or HCOC). Even though the LID design (bio-retention basins) adequately addresses the water quality requirements for the project, this design could still create streambed erosion. The project site is subject to hydromodification and thus source-control BMPs must also be used for the project to reduce flows. The on-site bio-retention basins will also serve as detention basins to mitigate post-development storm water runoff rates down to levels equivalent to the pre-developed condition, thus addressing the HCOC.

Additionally, source control BMPs will be used within the project, including permanent structural features with ongoing operations and maintenance. Some of the source control BMPs used include, "Only Rain Down the Storm Drain" stenciling on catch basins, litter control at truck docks, underground fire protection service and fire sprinkler tests, storm drain filters, landscape and irrigation, and sweeping in plaza/parking areas.

Hydromodification refers to changes in runoff characteristics caused by altered land use and increase of impervious areas.

Too much hydromodification can cause erosion of stream banks and beds. BMPs can include structural BMPs to reduce flows or volumes thus reducing impacts to downstream channels.





Source: Webb Associates

Figure 2-7 Drainage Plan

2.3.4 Grading

The grading plan for the site creates building pads for two buildings, two bio-retention basins, and a trapezoidal channel on the site's eastern edge. The grading plan includes approximately 562,116 cubic yards of cut, and 433,974 cubic yards of fill, inclusive of remedial grading (over-excavation).

Figure 2-8, *Conceptual Grading Plan*, illustrates the conceptual grading for the site. Final grading design and quantities will be based on final engineering. Any import or export of soil will be detailed on the Tentative Parcel Map for the project and in the final grading plans.

2.3.5 Dry Utilities

Natural Gas Service

The Southern California Gas Company provides natural gas service to the Specific Plan Area.

Electrical Service

Southern California Edison provides electrical service to the Specific Plan Area. Electrical service lines are present adjacent to the site to the north, associated with the March Air Museum. Electrical service will be extended to the Specific Plan Area along the Van Buren Boulevard extension to the proposed buildings.

2.3.6 Solid Waste

Solid waste generated on the Specific Plan area is currently collected by Waste Management Inc. (WMI).

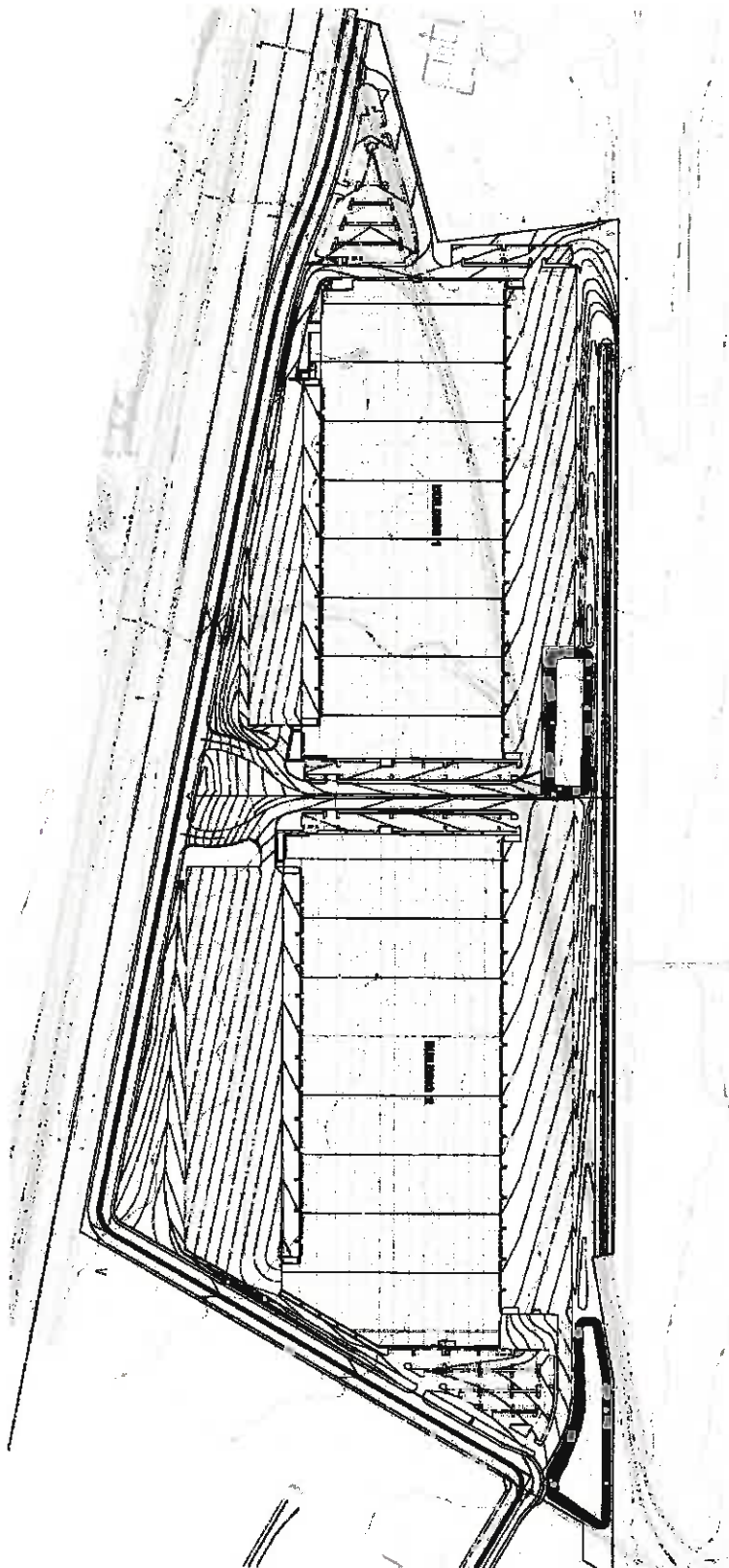
Solid waste in western Riverside County is disposed of at the El Sobrante, Lambs Canyon, and Badlands landfills. The majority of waste from the Specific Plan Area would be sent to the Badlands Landfill, located at 31125 Ironwood Avenue in Moreno Valley, with residual waste sent to the Lamb Canyon Landfill, located at 16411 Lamb Canyon Road in the unincorporated County of Riverside. Both landfills are owned and operated by Riverside County.

In order to reduce the amount of material generated by the Specific Plan's planned future development to meet the State's mandate of 50% solid waste diversion, the Specific Plan will comply with the requirements of the County of Riverside's Source Reduction and Recycling Element (SRRE) and the provisions of AB 341, which focuses on increased commercial waste recycling. Typical of large logistics uses, the logistics buildings will incorporate trash compacting areas.

2.3.7 Police and Fire Service

Law enforcement services in the March JPA planning area fall under the Riverside County Sheriff's Department. Sheriff substations are located within the cities of Moreno Valley, Riverside, and Perris.

Fire Services fall under the jurisdiction of the Riverside County Fire Department. Fire protection services are provided by existing County fire stations in Moreno Valley and non-County fire stations from the March Air Reserve Base and neighboring City of Riverside through mutual aid agreements. Existing County Station 6, located at 22250 Eucalyptus Avenue in Moreno Valley would provide fire response to the Specific Plan Area. Station 6 is located approximately six miles from the Specific Plan Area.



Source: Huitt-Zollars

Figure 2-8 Grading Plan

3

DEVELOPMENT REGULATIONS

This chapter discusses the general provisions and specific development standards for uses within the Specific Plan area, including setbacks, height, and parking requirements.

3.1 GENERAL PROVISIONS

The California Government Code (Title 7, Division 1, Chapter 3, Article 8, Sections 65450 et seq.) grants authority to cities and agencies to utilize Specific Plans for purposes of implementing the goals and policies of the agency's General Plan.

This Specific Plan establishes a set of regulations, standards, guidelines, and processes for the proposed development, and shall constitute the zoning for development within the Specific Plan area.

This section has been prepared in accordance with California Government Code Section 65450, et seq. and the March JPA Development Code (Section 9.13). Regulations are included for the proposed logistics land use.

Application of these regulations is specifically intended to provide the most appropriate use of the land, create a harmonious relationship among land uses and protect the health, safety and welfare of the community.

The following General Development Standards apply to all uses within the Specific Plan Area.

3.1.1 Applicability

The Veterans Industrial Park 215 Specific Plan has been developed as both a regulatory and a land use policy document, which, upon adoption by ordinance will constitute the zoning for the property. Development plans or agreements, tract or parcel maps, plot plans or any other action requiring ministerial or discretionary approval of the subject property must be consistent with the Specific Plan. California Government Code, Section 65454 requires that a Specific Plan be consistent with the General Plan. Upon adoption, actions deemed to be consistent with the Veterans Industrial Park 215 Specific Plan shall be judged to be consistent with the March JPA General Plan.

Where conflicts exist between the standards contained in this Specific Plan and those found in the March JPA General Plan or Development Code, the regulations and standards in this Specific Plan shall take precedence. Any area of site development, administration, review procedures, environmental review, landscaping requirements, and regulations not expressly addressed by this Specific Plan document shall be subject to the provisions of the March JPA Development Code, using the context and objectives of this Specific Plan as a guide.

3.1.2 Severability

In the event that any regulation, condition, program, portion or policy of this Specific Plan or the application thereof to any person or circumstance is held to be invalid or unconstitutional by any court of competent jurisdiction, such portions shall be deemed separate, distinct and independent provisions and shall not affect the validity of the remaining provisions of this Specific Plan or applications thereof which can be implemented without the invalid provision or application.

3.1.3 Consistency with Specific Plan

Properties within the Veterans Industrial Park 215 Specific Plan shall be developed in general conformance with the Land Use Plan (Figure 2-1, *Land Use Plan*). Development of properties governed by the Specific Plan shall be in accordance with the mandatory requirements of all March JPA ordinances (unless specifically revised herein) and state laws, and shall conform substantially to the Veterans Industrial Park 215 Specific Plan, as filed in the office of the March JPA Development Services Department, unless otherwise amended.

Except for the Specific Plan Development Standards/Design Guidelines and Substantial Conformance procedures adopted with the Veterans Industrial Park 215 Specific Plan, no portion of the Specific Plan which purports or proposes to change, waive, or modify any ordinance or other legal requirement for development shall be considered to be part of the adopted Specific Plan.

3.1.4 Subdivision Map Act

Lots created pursuant to the Veterans Industrial Park 215 Specific Plan and the concurrently processed tentative parcel map, shall be in conformance with the development standards of the zoning applied to the property and all other applicable JPA standards, as well as the Subdivision Map Act.

3.1.5 Determination of Unlisted Use

Any land use proposal not specifically covered by the provisions contained herein shall be subject to determination by the Community Development Director in accordance with Section 9.01.060 of the March JPA Development Code.

3.1.6 Interpretation

The development standards and regulations contained in this Specific Plan shall supersede the standards contained in the March JPA Development Code, except where specifically provided in this Specific Plan. Whenever the provisions contained in this Specific Plan conflict with the Development Code, the provisions of this Specific Plan shall take precedence. Any ambiguity concerning the content or application of the Specific Plan shall be resolved by the JPA's Planning Director or their designee in accordance with Development Code Section 9.01.060. Such interpretations shall take into account the stated goals and intent of this Specific Plan. If requested or appealed, the Commission may review any administrative interpretation.

3.1.7 Definitions

Unless otherwise specified below, terms used in this document shall have the same definitions provided in the JPA's Development Code. If a word is not defined in this section or in any provision of the Development Code, the Planning Director shall determine the correct definition.

- **E-Commerce.** E-Commerce is the buying and selling of goods and services over an electronic network, primarily the internet. This use includes internet fulfillment centers, in which orders are received from affiliated stores or other locations, processed, and filled. The number of employees, and therefore amount of employee parking, is higher than a high cube distribution center.
- **Mezzanine.** As defined herein, a mezzanine is an elevated, occupied floor above the ground floor of a larger industrial space used for office or other enclosed work space. For purposes of this Specific Plan, mechanical mezzanine platforms (typically relocatable, freestanding steel structures) used for logistics/high cube purposes are not counted in the square footage allocated to mezzanines in the land use table nor counted towards the maximum Floor Area Ratio (FAR) or parking counts.
- **High Cube Warehouse/Distribution Center.** High-cube warehouses or distribution centers are primarily for the storage and/or consolidation of manufactured goods (and to a lesser extent, raw materials) prior to their distribution to retail locations or other warehouses but may also accommodate minor assembly as an ancillary use. These facilities are generally very large buildings characterized by a small employment count due to a high level of automation/mechanization, and truck activities are frequently outside of the peak hours of the adjacent street system.
- **Warehousing, with Distribution** means the use of a building or buildings primarily for the interim (short-term) internal storage of goods of any type, which may include sales of goods (e.g. retail sales

or wholesaling). This use is generally engaged in receipt and distribution of goods, products, supplies, etc., with incidental storage and is typically identified with a quick turnaround of such goods.

3.1.8 Design Guidelines

Development shall be designed and built in substantial conformance with the Design Guidelines contained in this document (Refer to Section 4, *Design Guidelines*).

3.1.9 March Air Reserve Base Performance Standards

The Veterans Industrial Park is located in MARB Airport Influence Zone, therefore, all development within the plan shall comply with the following measures:

- **Avigation Easements.** Prior to recordation of a final map, issuance of building permits, or conveyance to an entity exempt from the Subdivision Map Act, whichever occurs first, an avigation easement shall be conveyed to March Air Reserve Base/March Global Port through the March JPA and will provide and disclose a "Notice of Airport in Vicinity" to building tenants.
- **Noise Standard.** All building office areas shall be constructed with appropriate sound mitigation measures as determined by an acoustical engineer or architect to insure appropriate interior sound levels. This standard will be confirmed as part of building permit plan check.
- **Retention and Water Quality Basins.** All retention and water quality basins shall be designed to de-water within 48 hours of a rainfall event.
- **Lighting Plans.** Prior to issuance of building permit, lighting plans shall be submitted to an airport lighting consultant or March Air Reserve Base/March Inland Port (MARB/MIP), for review and comment prior to issuance of building permits. Lighting shall consist of High Pressure Sodium or LED fixtures (below 2500 Kelvin).
- **Height Restrictions per Federal Aviation Regulations Part 77.** The Federal government has developed standards for determining obstructions in navigable airspace. Federal Aviation Regulations Part 77 defines a variety of imaginary surfaces at certain altitudes around airports. The Part 77 surfaces include the primary surface, approach surface, transitional surface, horizontal surface and conical surface. Collectively, the Part 77 surfaces around an airport define a bowl-shaped area with ramps sloping up from each runway end. The Part 77 standards are not absolute height restrictions, but instead identify elevations at which structures may present a potential safety problem. Penetrations of the Part 77 surface generally are reviewed on a case by case basis. The project area is located within the Inner Horizontal Surface (Surface E).

The Inner Horizontal Surface is a plane, oval in shape at a height of 150 feet above the established airfield elevation (i.e. 1,685 feet above MSL at the northern end of the runway and 1,638 feet above MSL at the southern end of the runway) extending 7,500 feet around the centerline of the runway. Therefore, structures would need to exceed an elevation of 1,685 feet MSL at the northern end of the runway and 1,638 feet at the southern end before they encroached into this Part 77 surface for the March Air Reserve Base runways.

3.2 PERMITTED USES

3.2.1 Permitted Uses

- a) Heavy and Light Logistics/distribution and warehousing, including high-cube warehousing (including uses requiring refrigeration of up to 10,000 square feet)
- b) E-Commerce, including fulfillment centers

- c) Research and Development
- d) Light Manufacturing and Assembly including aviation-related manufacturing

3.2.2 Ancillary Uses

An ancillary use is a permitted use which is subordinate to the primary permitted use. The following are uses which are permitted within the Specific Plan area as ancillary uses in support of and subordinate to the primary permitted uses.

- a) Cellular transition facilities and structures.
- b) Offices, including corporate, subsidiary and regional management offices.
- c) Maintenance facilities (internal) associated with a permitted use.
- d) Showrooms and retail uses not to exceed 5% of the gross building area.
- e) Outdoor vehicle, equipment and container storage ancillary to an approved use accommodated within an approved building (outdoor storage shall be screened when facing the public right-of-way).
- f) Short-term construction yards.
- g) Public utility uses and structures.
- h) Employee support uses including cafeteria/café and training facilities as an ancillary use.
- i) Uses determined by the Planning Director to be similar and not more intensive than other allowed ancillary uses.

3.2.3 Conditional Uses subject to further Environmental Review

- a) Aviation Related Facilities
- b) Facilities incorporating greater than 5% total building area in showroom/retail space.
- c) Cold Storage facilities larger than 10,000 S/F incorporating ammonia refrigeration or other refrigerants which are combustible or toxic.

3.2.4 Prohibited Uses

The following uses shall be prohibited within the Specific Plan:

- a) Outdoor new or used car, truck, trailer and equipment sales.
- b) Public Assembly facilities, inclusive of churches, assembly halls, schools, and libraries.
- c) Retail/Restaurant as a primary use.
- d) Above ground Petroleum Storage containers and below ground storage containers in excess of 10,000 gallons.
- e) Uses inconsistent with the B2 Airport Land Use Compatibility Zone as identified in the current Airport Land Use Compatibility Plan.
- f) Child care.
- g) Medical related facilities involving the treatment of patients.

- h) Solar panels: For projects that consider a roof or ground mounted solar photovoltaic system, a glare/glint study must be submitted to March ARB to examine the potential impacts on flight operations.
- i) Temporary Uses except for construction trailers and security offices.

3.3 DEVELOPMENT STANDARDS

No building or portion thereof shall be erected, constructed, converted, established, altered, enlarged, nor shall any legal lot or premises be used unless the legal lot or premises and building comply with the following regulations and standards. The following development standards are minimums unless otherwise stated.

Item	Dimension/Standard
Floor Area Ratio (maximum)	0.4 ¹
Front Setback	20 feet
Side Setback	5 feet
Rear Setback (at Property Line)	25 feet minimum
Buildings over 35 feet in height	1,224' feet minimum ² , subject to compliance with the 7:1 slope.
Building Separation	100 feet
Site Landscaping	10%
Building Height (maximum)	48 feet

3.3.1 Parking

Parking within the Veterans Industrial Park 215 Specific Plan area shall be provided in accordance with the ratios in Table 3-2, *Parking*:

Item	Parking Ratio
Office/Ancillary Retail	1 space/300 sf
Logistics ³	
0-50,000 sf	1 space/1,000 sf
50,000 – 200,000 sf	1 space/3,000 sf
200,000 + sf	1 space/ 5,000 sf
Bicycle Parking	1 /20 auto stalls/5%
Carpool Stalls	5% of auto stalls
Electric Vehicle Charging Stations	2.5% of auto stalls or per CAL Green requirements

¹ Floor Area Ratio (FAR) shall be averaged - calculated across the entire Specific Plan Area. FAR shall not include mechanical mezzanines.

² Measured from runway centerline.

³ Mechanical mezzanines shall not require additional parking.

3.3.2 General Design Standards

This section is intended to provide the general development regulations and standards for land uses located within the Specific Plan area. The following standards shall apply:

1. Buildings containing the land uses shall consist of quality architectural features.
2. Architectural elements such as pilasters, columns, canopies, porticos, colonnades, arcades, and other architectural elements may be incorporated.
3. In addition to the architectural elements standards expressed in this subsection, color changes, texture changes, and material changes shall be used.
4. Methods to reduce the likelihood of graffiti, such as creeping vines or other methods shall be incorporated, as appropriate.
5. Building entryways shall be clearly defined and incorporate architectural details.

Trash, Service, and Delivery Areas

1. Service areas and loading docks shall be screened from view from adjacent streets.
2. All outdoor storage areas for equipment shall be fully screened from view.
3. When appropriate, a landscape buffer may be provided along service/delivery areas.

Trash enclosures shall be a minimum six (6) feet in height and should be architecturally compatible with the main building.

Mechanical Equipment

1. Rooftop mechanical equipment shall be securely fastened to the roof and fully screened with architectural elements consistent with the overall design of the primary structure.
2. Exterior ventilating and mechanical equipment shall not disturb neighboring occupants and shall be screened, shielded, and/or buffered from sound from adjacent properties.
3. All mechanical equipment, including aboveground utility boxes, telephone boxes, back flow preventers, cable boxes, or similar structures shall be fully screened from view from the closest adjacent public street. Screening shall not obstruct required equipment access required by the relevant utility provider.
4. Satellite dishes shall be roof-mounted and screened from view.

Lighting and Security

1. Site lighting shall be low or high-pressure sodium, maximum 750-watt, full cut-off fixtures, with the maximum light fixture height of 25 feet above finished grade, and a maximum lighting level of 0.5 foot candles at the property line. For LED lighting an equivalent wattage level shall be provided.
2. All freestanding light poles shall be located within landscaped areas. Concrete light pole bases shall be painted to match the primary building color or finished to match parking screening walls and shall not exceed 24' above finished grade.
3. No cameras may be oriented towards the runway and cameras must not record base airfield operations.
4. Perimeter fencing adjacent to airport runway must be a minimum of eight feet in height with three strands of barbed wire.

4

DESIGN GUIDELINES

This chapter explains design concepts and establishes design policies and design guidelines for development within the Specific Plan area. These guidelines address the built form as well as general guidelines related to mobility and parking, landscaping and signage.

The guidelines within this chapter describe and illustrate building designs, concepts, and features that will promote the high-quality development that is envisioned for the Specific Plan area. The design guidelines should be used in conjunction with the development standards described in Chapter 3: Development Regulations.

These design guidelines will serve to promote cohesive design and community identity. Graphics and photographic images are included as a visual reference and should not be interpreted as the only design solution. Creative approaches are encouraged.

These Design Guidelines serve the following functions:

- To provide the March JPA with assurance that the Veterans Industrial Park will be developed in accordance with the quality and character described within this Specific Plan.
- To establish design guidelines for site design, architecture, circulation, parking, lighting, and other distinguishing features.
- To provide guidance to JPA staff, and the Commission in the review of future implementing projects within the Specific Plan area
- To provide developers, builders, planners, architects, landscape architects and property owners with guidelines and recommendations, to aid in maintaining the high level of community cohesiveness and unity, while still allowing for a degree of personal expression.
- Encourage sustainable design solutions that reduce energy consumption, use water efficiently, and minimize waste.
- Create simple building designs that result in efficient use of space, materials, and resources while maintaining a high level of design integrity and authentic architectural style.

The terms “shall”, “should”, and “may” are used within the Design Guidelines. The term “shall” is used to denote a design standard where compliance is required. The term “should” is used to denote a guideline that is recommended, but not required in all circumstances. The term “may” is used to denote a design treatment that is allowed or optional.

These guidelines may be subject to modification over time to respond to unanticipated conditions, such as changes in the real estate market, specific needs of buildings users, technology advancements, and economic fluctuations.

4.1 INTRODUCTION

These Design Guidelines are intended to create quality development while allowing flexibility. Projects implementing this Specific Plan will depict detailed building footprints, parking lot layouts, internal circulation flow patterns, and landscaping, and should be in substantial conformance with the goal of these Design Guidelines. However, the Design Guidelines in this Section are not intended to be interpreted in a way that would unnecessarily burden the Developer(s) and their design professionals with the need to exactly replicate the exhibits included in these guidelines.

These Design Guidelines consist of two principal elements: Architecture and Landscape. These elements define the design concept, physical character, and overall theme of the Veterans Industrial Park. Text descriptions and graphic exhibits are used to convey the overall theme of the project.

The Architectural Design Guidelines address the industrial themed architecture for buildings permitted within the Veterans Industrial Plan and are intended to provide a basis for decisions regarding the

structural environment. A high-quality industrial project is defined by the guidelines provided for architectural design and details, building mass and scale, materials and exterior colors, and articulation.

The Landscape Guidelines present general landscaping requirements, including streetscape design, entry treatments, signage, water quality features, walls and fencing, and lighting. Plant material guidelines provide direction regarding the use of plant materials that complement the overall theme. The Landscape Design Guidelines also provide general requirements relating to water conservation.

4.2 ARCHITECTURE GUIDELINES AND STANDARDS

4.2.1 Building Form and Orientation

Building form is a defining feature of architecture. Shape, massing, scale, proportion, and articulation are all components of a building's form. The proposed architecture for the Veterans Industrial Park is a contemporary design appropriate to the proposed industrial use. Building Corners facing Van Buren Boulevard will be utilized for offices and show a higher level of articulation and fenestration than the logistics/warehouse components of the plan.

Figure 4-1, *Example Architecture*, illustrate elevations that comply with these design guidelines. Future building designs may vary from this example. Buildings within the Veterans Industrial Park 215 Specific Plan shall comply with the following guidelines:

- Buildings should be oriented so that loading areas are screened from view from streets and public areas.
- Buildings should be arranged to provide convenient access to entrances and efficient internal circulation for vehicles and pedestrians.
- Visitor parking should be located with convenient access to public building entries
- Indoor or outdoor break areas shall be provided.
- Architectural style should be of a classic, contemporary technical/industrial style with clean efficient lines. Simple geometric forms shall constitute the overall building form. Rectangular forms are encouraged to promote balance and visual interest. Arbitrary, complicated building forms and rooflines should be avoided.
- Building planes visible from Van Buren Boulevard should be articulated using changes in building materials, color, and/or decorative accents/scoring.
- Modulation and variation of building masses between adjacent buildings visible from Van Buren Boulevard or Western Way is encouraged.
- Materials applied to any elevations shall turn the corner of the building to a logical termination point in relation to architectural features or massing.
- Pedestrian entrances to buildings accessible to visitors should be identifiable through changes in massing, color, and/or building materials.
- Primary building entries shall be easily identified through the use of prominent architectural elements, signage, landscaping, lighting, canopies, roof form, hardscape, architectural projections, columns, vertical elements, or other design features that help emphasize the building's entry.





Source: RGA

Figure 4-1 Example Architecture

4.2.2 Materials and Colors

Complementary materials and colors play a key role in developing a pleasing visual environment. Slight variations from building to building are permitted within the Specific Plan area to provide visual interest.

- Materials shall be of a non-reflective material, including exterior ductwork, windows, and roofs.
- Appropriate primary exterior building materials within the Specific Plan area include tilt-up concrete panels, stucco, and concrete.
- Primary materials may be accented by secondary materials on elevations visible from public streets such as Van Buren Boulevard and Western Way. Appropriate secondary materials may include glass, natural or fabricated stone, metal, and tile or tile panel systems. Highly reflective materials on elevations facing the runway or the aircraft approach path are prohibited.
- Building materials shall be durable and able to withstand long-term exposure to the elements.
- Trim details may include metal finished in a consistent color, plaster, or concrete elements finished consistently with the building treatment. Foam cornice caps or moldings are discouraged.
- Colors and materials for all structures onsite should consist of earth tones. Use of at least two to three different colors, materials or textures is encouraged. Bright, primary colors are discouraged, except in tenant signage logos.
- Large expanses of smooth material (e.g., concrete) shall be broken up with expansion joints, reveals, or changes in texture and color.
- The color of exposed downspouts, service doors and mechanical screens should complement the color of the structure.

4.2.3 Windows and Doors

Windows and doors should be defined by function, consistent in form, pattern, and color. Appropriate treatments consist of functional glass use, a balance of glazing and wall surfaces, with no highly reflective surfaces facing the runway.

- Window layout should be in a repetitive pattern for visual continuity.
- Window and door styles and trims should be consistent within a building and among multiple buildings.
- Mirrored or highly reflective glass is not permitted.
- Pedestrian entries should be clearly defined.

4.2.4 Loading Docks and Service Doors

- Service doors, loading docks, and truck courts should be screened so they are not easily visible from public roads, unless the public road is substantially higher in elevation than the loading areas. Screening may be accomplished with solid walls compatible with the architectural style of the building or by a combination of screen walls, landscaping, and berms. Screen walls may be located at the foot or top of slopes to effectively screen loading areas.
- Docks and truck courts should be separated from visitor and customer parking areas and pedestrian walkways through the use of walls, fences and/or landscaping.

- No loading or unloading activity is permitted to take place from public streets or the internal drive aisles. Trucks shall have clear and convenient access into and within the truck courts of each building and should not disrupt vehicular and pedestrian circulation.

4.2.5 Security Elements

Cameras

The location and appearance of security cameras must be integrated with the architecture. The top of any roof-mounted camera must be below the top of the parapet.

- No cameras may be oriented towards the runway and cameras must not record base airfield operations.
- Cameras may be mounted on poles in parking lots
- Cameras may be mounted on building or screen walls with the top of the camera below the top of the parapet
- The color of the camera housing should match the color of the poles or the building wall.

Inappropriate Treatment

- Wall-mounted cameras with the top of the camera above the top of the parapet
- Exposed wiring
- Cameras mounted in spheres on arms projecting from building walls.

Fencing

Along the runway/airport boundary on the Specific Plan area's eastern edge, special security fencing shall be used. Fencing must be a minimum eight feet in height with three strands of barbed wire. This fencing shall be of a durable material (may be chain link) subject to JPA and March Air Reserve Base review.

Gating

Pedestrian and vehicular access gates visible from public areas (i.e., parking lots, streets, sidewalks, etc.) shall be constructed of a durable material, such as tubular steel.

4.2.6 Trash Enclosures

- All outdoor refuse bins or other containers must be screened within a permanent, durable enclosure and oriented away from public roads or other public view.
- The design of trash enclosures must be consistent with the architectural style, color, and materials of adjacent buildings.
- At least one trash enclosure shall be located adjacent to each building. Three sides of the trash enclosures will be constructed of concrete or block walls and the fourth side of a gate.

4.3 LANDSCAPE GUIDELINES

4.3.1 Landscape Master Plan

Landscape treatments around buildings will be designed to help break up the building massing by incorporating both tall, vertical trees and lower growing and broader canopy trees along Van Buren Boulevard. The ground plane will be landscaped with a mix of shrubs and ground cover plants to create a layered appearance. A Conceptual Landscape Plan for the Specific Plan area is illustrated on Figures 4-3,

and 4-4 *Landscape Master Plan*. Trees used within the Specific Plan will be selected from the list on Table 4-1 *Plant Materials*. Shrubs and groundcovers will be selected concurrent with final designs for individual projects within the Specific Plan area.

- Landscaping shall be provided in all setback areas of the Specific Plan area.
- The Specific Plan area shall comply with the landscape design measures to reduce water use contained within the MIPA Development Code Section 9.17.
- Streetscapes will incorporate low water use plant materials to minimize irrigation needs. Open space areas not planted with living material should utilize permeable materials such as decomposed granite, mulch and/or rocks/cobble to reduce irrigation demands where possible.
- The community shall be irrigated with reclaimed or recycled water if available.
- Planting areas will be irrigated with a high efficiency automatic irrigation system.



Figure 4-2 Van Buren Boulevard Streetscape Edge

4.3.2 Water Quality

Bio-retention basins are included in the drainage plan for the Specific Plan. The basins are shallow impoundments designed to collect, treat, and detain stormwater runoff before discharging it. In addition to functioning as storm water/water treatment facilities, water quality features such as bioswales need to consider their impact on the overall aesthetics of the project. Whenever possible, these facilities will be carefully sited and integrated into landscaped areas if possible, and to avoid the look of engineered, utilitarian facilities. The design should be integrated into the landscape and appear as a landscape feature, while incorporating acceptable plant material.



Infiltration Basin (Source: SoCal LID Manual)

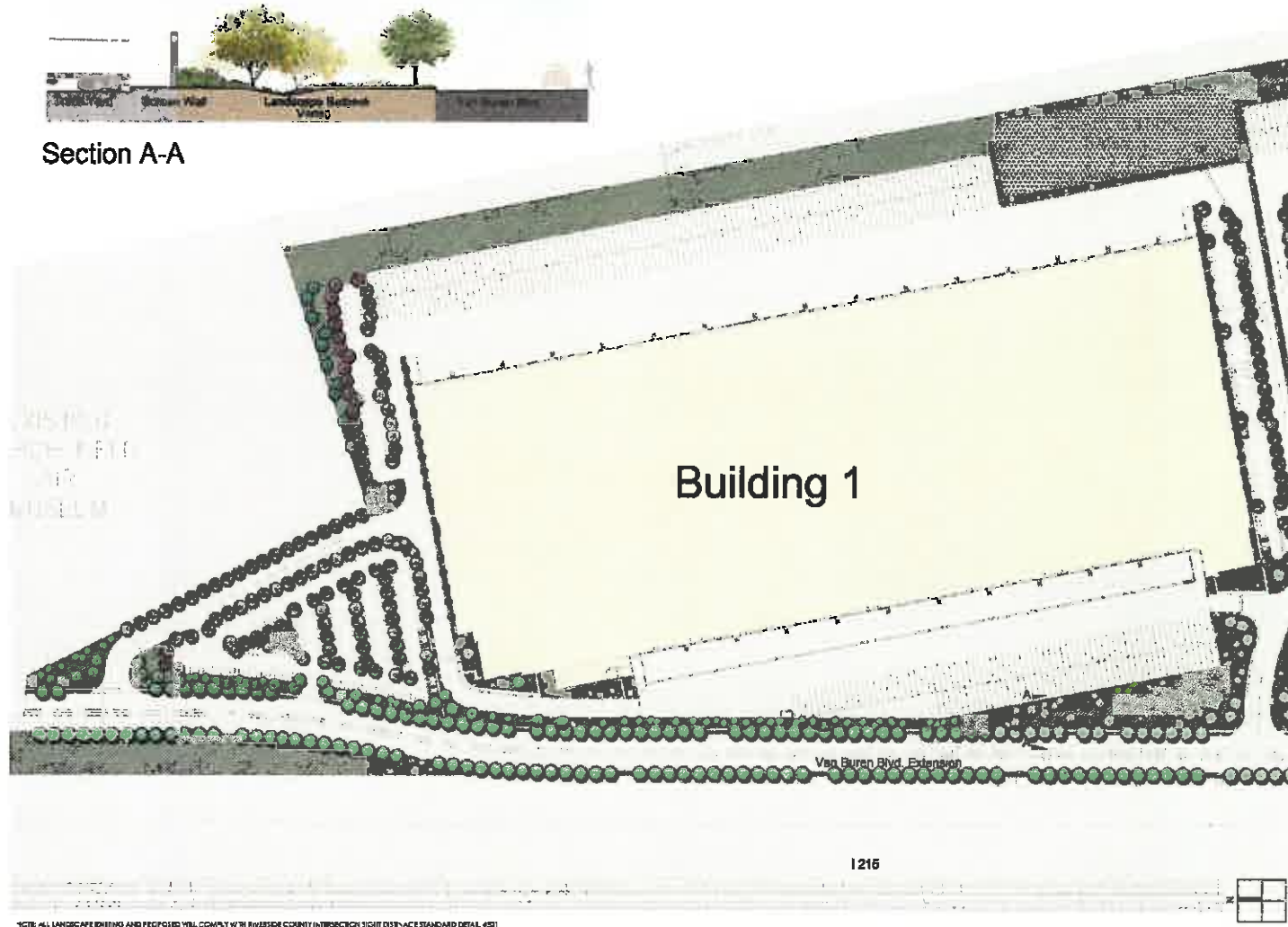
Vegetative cover is important to minimize erosion and ensure that treatment occurs in biofiltration basins. To prevent basins from being used as walkways or passive recreation areas to the extent that their primary function is compromised, bioretention facilities will be planted with a combination of small trees along the upper edges, densely planted shrubs, and natural grasses (see Table 4-2).

The following guidelines should be followed:

- The basin area should be designed for at least 70 percent mature coverage to maximize biofiltration. Basin bottoms shall be constructed of porous material to allow for water penetration.
- Grasses should be native or ornamental; preferably ones that do not need to be mowed.
- The application of fertilizers and pesticides should be minimal.
- To maintain oxygen levels for the vegetation and promote biodegradation, and to comply with aviation-related restrictions related to ponding, it is important that vegetation not be completely submerged for any extended period of time.

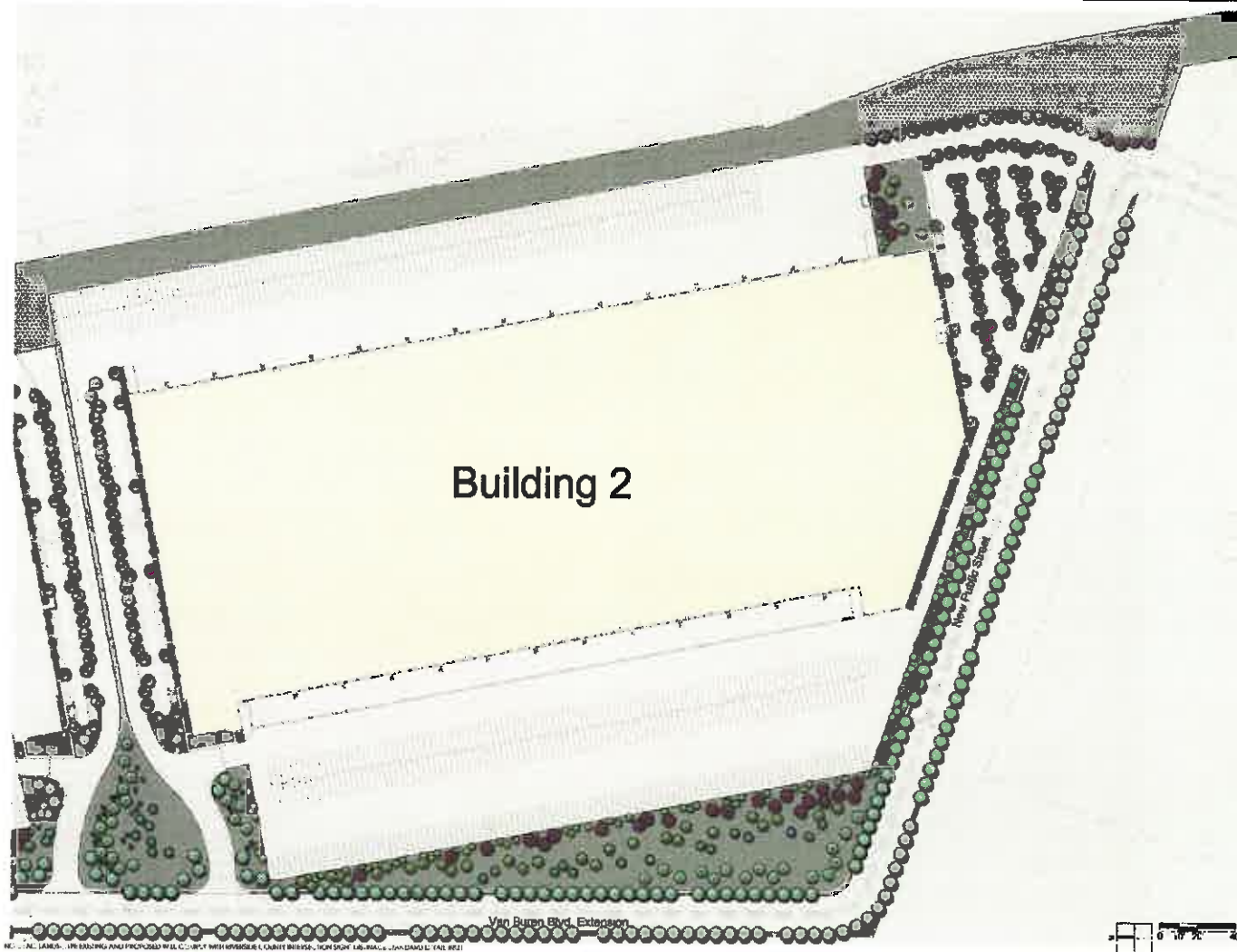
Botanical Name	Common Name	Size	WUCOLS	Remarks
<i>Cercidium</i> 'Desert Museum'	Blue Palo Verde	24" Box	L	Multi
<i>Chilopsis linearis</i>	Desert Willow	24" Box	L	Multi
<i>Chitalpa tashkentensis</i>	Chitalpa	24" Box	L	Standard
<i>Prosopis</i> spp.	Mesquite Tree	24" Box	M	Standard
<i>Acacia smallii</i>	Acacia	24" Box	L	Multi
<i>Acacia stenophylla</i>	Shoestring Acacia	24" Box	L	Multi
<i>Pinus canariensis</i>	Canary Island Pine	24" Box	M	Standard
<i>Pinus eldarica</i>	Afghan Pine	24" Box	L	Standard
<i>Rhus lancea</i>	African Sumac	24" Box	L	Standard
<i>Tristania conferta</i>	Brisbane Box	24" Box	M	Standard
<i>Washingtonia robusta</i>	Mexican Fan Palm	10' bt	L	Skinned
<i>Braha armata</i>	Blue Fan Palm	15 Gal	M	Standard

Botanical Name	Common Name	Size	WUCOLS	Remarks
<i>Cistus</i> 'Sunset Pink'	Sunset Pink Rockrose	5 Gal	M	
<i>Leucophyllum</i> spp.	Texas Ranger	5 Gal	L	
<i>Ligustrum j. Texanum</i>	Texas Privet	5 Gal	M	
<i>Salvia c. 'Allen Chickering'</i>	Allen Chickering Sage	5 Gal	L	
<i>Salvia greggii</i>	Autumn Sage	5 Gal	L	
<i>Slavia leucantha</i>	Mexican Sage	5 Gal	L	
<i>Senna artemisioides</i>	Feathery Cassia	5 Gal	L	
<i>Caesalpinia pulcherrima</i>	Red Bird of Paradise	5 Gal	L	
<i>Tecoma stans</i>	Yellow Bells	5 Gal	L	
<i>Artemisia</i> spp.	Wormwood	5 Gal	L	
<i>Rhamnus</i> spp.	Coffeeberry	5 Gal	L	



Source: Hunter Landscape

Figure 4-3 Planning Area 1 Landscape Plan



Source: Hunter Landscape

Figure 4-4 Planning Area 2 Landscape Plan

Table 4-1 (Cont'd) Plant Materials - Accents				
Latin Name	Common Name	Size	WUCOLS	Remarks
Agave spp.	Agave	5 Gal	L	
Aloe spp.	Aloe	5 Gal	L	
Dasylerion wheeleri	Desert Spoon	5 Gal	L	
Hesperaloe parviflora	Red Yucca	5 Gal	L	
Lantana 'Gold Mound'	Yellow Lantana	5 Gal	L	
Baileya multiradiata	Desert Marigold	5 Gal	L	

Table 4-1 (Cont'd) Plant Materials - Groundcovers					
Latin Name	Common Name	Size	Spacing	WUCOLS	Remarks
Acacia redolens 'Low Boy'	Dwarf Acacia	1 Gal	8' O.C.	L	
Baccharis p. 'Pigeon Point'	Dwarf Coyote Bush	1 Gal	6' O.C.	L	
Baccharis p. 'Centennial'	Coyote Bush	1 Gal	48" O.C.	L	
Carex pansa	California Meadow Sedge	4" Pots	12" O.C.	M	Grass
Carex tumulicola	Foothill Sedge	1 Gal	18" O.C.	M	Grass
Festuca mairei	Altas Fescue	1 Gal	24" O.C.	M	Grass
Festuca o. 'Gluca'	Blue Fescue	1 Gal	12" O.C.	M	Grass
Hemerocallis hybridus-Yellow	Yellow Day Lily	1 Gal	24" O.C.	M	
Lantana 'Gold Mound'	Yellow Lantana	1 Gal	36" O.C.	L	
Lonicera j. 'Halliana'	Hall's Honeysuckle	1 Gal	48" O.C.	L	
Muhlenbergia capillaris	Pink Muhly	1 Gal	36" O.C.	L	Grass
Myoporum parvifolium	Myoporum	1 Gal	36" O.C.	L	
Nassella tenuissima	Mexican Feather Grass	1 Gal	24" O.C.	VL	Grass
Pennisetum messiacum	Red Bunny Tails Fountain Grass	1 Gal	30" O.C.	L	Grass
Pennisetum a. Little Bunny	Little Bunny Fountain Grass	1 Gal	12" O.C.	L	Grass
Pennisetum orientale	Oriental Fountain Grass	1 Gal	30" O.C.	L	Grass
Pennisetum rubrum	Purple Fountain Grass	1 Gal	36" O.C.	L	Grass
Rosmarinus o. 'Huntington Carpet'	Prostrate Rosemary	1 Gal	48" O.C.	L	
Salvia 'Bees Bliss'	Bee's Bliss Sage	1 Gal	48" O.C.	L	
Senecio mandraliscae	Blue Fingers	4" Pots	12" O.C.	M	
Sesleria autumnalis	Moor Grass	1 Gal	18" O.C.	M	Grass

Table 4-2 Bioretention Basin Seed Mix	
Latin Name	Common Name
Achillea millefolium	Yarrow
Eschscholzia caespitosa	Foothill Poppy
Juncus bufonius	Toad Rush
Leymus tritcoides Rio	Wild Rye
Descampsia cespitosa	Tufted hairgrass
Festuca rubra "Molate"	Red Fescue
Hordium brachyantherum	Meadow Barley
Muhlenbergia rigens	Deergrass

4.3.3 Utility Placement and Screening

All exterior ground-mounted equipment—including, but not limited to, mechanical equipment, electrical equipment, emergency generators, boilers, storage tanks, risers, electrical conduit, gas lines, cellular telephone facilities, and satellite dishes must be screened from on-site and off-site view by a combination of decorative walls (where appropriate) and dense landscaping.



- Utility boxes should be grouped where possible and placed in landscape setbacks and/or shrub/groundcover areas.
- Above grade utility boxes should be screened and planted to the extent possible while allowing required access and clearance, and providing for adequate sight distance if located near intersections.

Inappropriate Screening Treatments

- Screening materials contrasting with adjacent structures
- Chain link fencing
- Lack of landscape buffering

Roof-mounted mechanical equipment shall be fully screened by a parapet wall equal to or exceeding the height of the mechanical units, subject to FAA height limits.

4.3.4 Walls and Fences

Walls and fences must be designed as an integral part of the overall architectural or landscaping design concept. Within designated edge treatment areas, proposed fencing shall be included in the required Concept Plan.

- Along the runway/airport boundary on the Specific Plan area’s eastern edge, special security fencing shall be used. Fencing must be a minimum eight feet in height with three strands of barbed wire. This fencing shall be of a durable material (may be chain link) subject to MIPA and March Air Reserve Base review.

- Six-Foot Tubular Steel Fences are provided around the Water Quality Basins to provide safety and security for pedestrians walking near the Basins. The Tubular Steel Fence is constructed from steel pickets painted black.
- Screening walls for trucks shall be 12' high (minimum).

Pedestrian and vehicular access gates visible from public areas such as parking lots and public streets shall be constructed of a durable material, such as tubular steel.

Plot Plans must include all site fencing, truck screening wall, and gate details.

Materials

Walls are to be constructed of materials compatible with the overall design character of the building. Walls shall be poured-in-place concrete or painted tilt-up screen walls. Fences shall be wrought iron or tubular steel. Chain link fencing is not permitted except for security fencing along the runway.

Design elements may include:

- Varied heights
- wall plane offsets
- Scoring or other decorative elements
- Pilasters or distinctive elements.
- Minor changes of material and finishes where appropriate.
- Trellis/vine panels or landscape pockets.



4.3.5 Exterior Lighting

Lighting will utilize high efficiency technologies, dark-sky cutoffs, strategic orientation to avoid spillover into adjacent properties, the adjacent runway, and open space areas, and appropriate shielding or recesses to minimize glare and reflections.

Street and parking lot lighting will meet JPA standards.

- Exterior lighting should be unobtrusive and not cause glare or spillover into neighboring properties, especially when within 100 feet of open space or adjacent runways. Lighting fixtures must be fully shielded to direct illumination downward to minimize light pollution impacts.
- Adequate lighting should be provided throughout the site to create an inviting and non-threatening environment. Night lighting of public spaces should be kept to the minimum necessary for safety and security purposes.
- The scale, materials, colors, and design detail of on-site light posts and fixtures should reflect the desired character of the Specific Plan area and the architectural style of the surrounding buildings. Light posts should be appropriately scaled to pedestrians near sidewalks and other areas of pedestrian circulation. Extremely tall light posts and fixtures should be avoided – maximum height is 25 feet. Bollard lighting is encouraged to illuminate walkways without providing spillover.
- Lighting fixtures should be compatible with the architectural style and character of the building. The color, size, placement, and number of fixtures should enhance the overall design and character of the building and site.
- Energy efficient, low voltage lighting is strongly encouraged. Decorative lighting should be low intensity. LED lighting below 2500 Kelvin is also allowed.
- If security lighting is required, fixtures should be hooded, recessed, and/or located in such a manner to only illuminate the intended area.
- Addresses should be visible from streets and illuminated at night.

4.3.6 Signage

Signage will be provided in accordance with a Sign Program prepared prior to building permit issuance. The Signage will conform to MIPA Development Code requirements.

5

ADMINISTRATION AND IMPLEMENTATION

This chapter discusses the development review procedures by the March JPA and other relevant permitting agencies applicable to the Specific Plan. A process for amendments to the Specific Plan is discussed as well as a process for substantial conformance determinations.

The purpose of this chapter is to provide an outline of the steps necessary to implement the Veterans Industrial Park 215 Specific Plan and applicable regulations in coordination with the March JPA and other governing public agencies. This chapter is intended to address each of these elements for the benefit of the development team, the MIPA and other relevant agencies, and interested citizens.

The approval of this Specific Plan, certification of an Environmental Impact Report, and adoption of conditions of approval and a Mitigation Monitoring and Reporting Program (MMRP) will assure that timely mitigation of project impacts takes place at the appropriate milestones and in accordance with project implementation.

5.1 ADMINISTRATION

California Government Code (Title 7, Division 1, Chapter 3, Article 8, Sections 65450 et seq.) grants authority to agencies to utilize Specific Plans for purposes of implementing the goals and policies of the General Plan.

This Specific Plan establishes a set of regulations, standards, guidelines, and processes for the proposed development, and shall constitute the zoning for development within the Specific Plan area.

5.1.1 Responsibility

The March JPA's Planning Department, its Director or their designee shall be responsible for administering the Veterans Industrial Park 215 Specific Plan in accordance with the provisions of this Specific Plan document, all governing and applicable state and federal laws, the March JPA General Plan, and the March JPA Development Code.

5.1.2 Applicability

The Veterans Industrial Park 215 Specific Plan serves as the implementation tool for the zoning for the Specific Plan Area. The Specific Plan addresses permitted uses, development standards, and design guidelines.

5.1.3 Enforcement and Interpretation

The JPA shall enforce the provisions of the Specific Plan in the same manner that it enforces the provisions of the General Plan and Development Code.

Whenever the provisions contained in the Specific Plan conflict with the Development Code, the provisions of the Specific Plan shall take precedence. Any ambiguity concerning the content or application of the Veterans Industrial Park 215 Specific Plan shall be resolved by the Planning Director, or their designee. Such interpretations shall take into account the stated goals and intent of the Specific Plan.

5.1.4 Severability

If any portion of this Specific Plan and its regulations are declared to be invalid or ineffective in whole or in part by a court of competent jurisdiction, such decision shall not affect the validity of the remaining portions thereof.

5.1.5 Initial Entitlements

Initial entitlements required for development of the Specific Plan area include the following actions to be taken by the JPA:

- Environmental Impact Report ("EIR") – The Veterans Industrial Park 215 Specific Plan is a discretionary project and is subject to the requirements of the California Environmental Quality

Act (“CEQA”). As part of the approval process for the Specific Plan, an Environmental Impact Report must be considered and certified by the JPA prior to approval of any of the project-related entitlements.

- **General Plan Amendment** – The Project site is presently identified as “Aviation” by the General Plan. An amendment of the General Plan Land Use map will be required to add a “Specific Plan” designation (SP-8) to the underlying Aviation designation. In addition, an amendment to the list of uses within the designation would include the addition of general warehousing/logistics.
- **Specific Plan** – The Project site is presently un-zoned. The Veterans Industrial Park 215 Specific Plan is a regulatory document that establishes the zoning, land use designations, development standards, and design guidelines for the entire Specific Plan project area. The Specific Plan will implement the JPA’s General Plan. The Specific Plan will be considered by the Joint Powers Commission (JPC) and will be adopted by Ordinance. Tract/parcel maps or plot plans must be in substantial compliance with the adopted Specific Plan.
- **Plot Plan**. The site development plan for the project, consisting of two buildings of approximately 2,185,618 square feet, parking, landscaping, drainage facilities, and new streets.
- **Subdivision Map** – The Subdivision Map is a basic tool for implementation of a Specific Plan. The project’s Tentative Parcel Map will create the individual lots for project development, formalize the parcel boundaries, and provide for public rights-of-way for Project access. A Tentative Parcel Map has been prepared (TMP 37220) and will be considered by the JPA concurrently with the review of this Specific Plan. The Parcel Map creates the backbone road rights-of-way, and two development parcels.
- **Development Agreement/Disposition and Development Agreement** – A statutory development agreement, authorized pursuant to California Government Code Section 65864 et seq., will be processed as part of the approval of this Specific Plan. The development agreement of this Specific Plan will include, among other items, methods for financing acquisition and construction of infrastructure. Such development agreement shall be fully approved before the issuance of the first building permit for this project. In addition, and Disposition and Development Agreement (DDA) will be executed to formalize the land transaction.

5.1.6 Substantial Conformance

Final development plans for the project may be adjusted or modified based on final design and engineering and the precise development plans of the builder. Substantial Conformance is a mechanism to allow the approval of minor modifications for development under the Specific Plan.

Written documentation requesting a proposed minor modification to support an implementing map, site plan, or use permit or modification of conditions of approval must be submitted for the review and approval of the Planning Director or their designee in accordance with Section 9.02.280 and Section 9.02.290 of the March JPA Development Code.

A Substantial Conformance application shall be subject to minor development review procedures. A substantial conformance application may be filed in lieu of an applicable minor development review application, provided that the proposal complies with the limitations described below:

- That the proposal is not inconsistent with the expressed intent of the original project approval;

- That the proposal qualifies as a categorical exemption under the California Environmental Quality Act and/or the proposal is consistent with the environmental determination for the original project and where no further environmental determination is necessary; and
- That the proposed modifications do not have the potential to adversely affect surrounding land uses or improvements.

5.1.7 Amendments

Substantial modifications to the Specific Plan would require an Amendment. A minor modification or adjustment to the Specific Plan listed in the section above would not require a Specific Plan Amendment.

An amendment to the Specific Plan is required if the following occur:

- Changes to the overall Specific Plan boundaries to include ownerships or properties not included in the Specific Plan at the time of approval (changes to planning area boundaries within the Specific Plan boundaries are deemed minor as noted above and would not require an amendment);
- Any increase in the overall development intensity thresholds within the Specific Plan; or
- Any addition of new land uses not contemplated by the Specific Plan's Development Regulations.

5.1.8 Appeals

An appeal of any determination, decision, or requirement of the March JPA Planning Director shall be made in conformance with the appeal procedures established by the Development Code Section 9.02.240.

5.2 IMPLEMENTATION

5.2.1 Adoption

The Veterans Industrial Park 215 Specific Plan will be prepared, submitted, and approved in a manner consistent with California Government Section 65451, as well as Chapter 9.13 of the JPA's Development Code. The Specific Plan will be adopted by Ordinance and shall serve as the zoning for the Veterans Industrial Park 215 project area. The approved Specific Plan project site will be designated on the JPA's General Plan Land Use Diagram and Zoning Map as the Veterans Industrial Park 215 Specific Plan. The land use and development standards identified in this Specific Plan document supersede all zoning regulations to the extent that they would be in conflict with the sections of this Specific Plan.

5.2.2 Phasing

Construction of the proposed project, including recordation of final subdivision map(s), and plot plan review may be progressively implemented in stages, provided that vehicular access, public facilities, and infrastructure are constructed to adequately service the development, or as needed for public health and safety.

Any project phasing would:

- Provide for the orderly build-out based upon market demand;
- Provide adequate infrastructure to service the project;
- Phases may occur concurrently so long as the associated infrastructure is provided.

5.2.3 Maintenance and Ownership

Maintenance of facilities within the Veterans Industrial Park 215 Specific Plan will be accomplished through a combination of public and private mechanisms. Generally, facilities dedicated to public agencies will be maintained by that agency, while private facilities will be maintained by a private maintenance mechanism. Table 5-1, *Financing, Ownership, and Maintenance* outlines the anticipated program.

A Business Association and/or multiple associations may be formed to address the maintenance of private drives, shared driveways, landscaping, signage, water quality features, and private infrastructure within the Specific Plan.

Table 5-1 Financing, Ownership, and Maintenance			
Improvement	Financing	Ownership	Maintenance
Water System (off-site)	Developer	Public	Public
Water System (on-site)	Developer	Private	Private
Sewer System (off-site)	Developer	Public	Public
Sewer System (on-site)	Developer	Private	Private
Drainage System On-site Regional	Developer Developer	Private Public	Private Public
Public Street Improvements	Developer	MJPA	MJPA
Private Internal Streets and driveways	Developer/Builder	Private	Private
Landscaping within Public Right-of-Way	Developer	MJPA	MJPA/Private

5.2.4 Relationship to CEQA

The California Environmental Quality Act (CEQA) classifies a specific plan as a “project” which is subject to environmental review. An Environmental Impact Report (EIR) is required prior to adoption of this Specific Plan to analyze potentially significant environmental impacts of the project, discuss feasible alternatives, and recommend feasible mitigation measures in compliance with the provision of CEQA. This EIR will analyze the Specific Plan and address potential impacts associated with the development of the Specific Plan area. The EIR will include recommended mitigation measures and analyzes implementing actions for the development. The EIR will fulfill the requirements for environmental documentation for most subsequent discretionary and ministerial applications for development within the Specific Plan area.

An approved Mitigation Monitoring Program will insure that the Specific Plan complies with all applicable environmental mitigation and permit requirements. The final approved Mitigation Monitoring program shall be established upon EIR certification by the MJPA.

6 APPENDICES

- Legal Description
- General Plan Conformance
- Land Use Compatibility Plan

**APPENDIX A
LEGAL DESCRIPTION**

THE LEGAL DESCRIPTION SHOWN HEREON HAS BEEN PREPARED BY THE SURVEYOR OF RECORD FOR THE PURPOSE OF DEPICTING THE AREA TO BE INCLUDED IN THIS SURVEY ONLY. SAID DESCRIPTION IS NOT TO BE USED FOR CONVEYANCE OF TITLE OF ANY NATURE.

THAT PORTION SECTIONS 25, 26, 35 AND 36, TOWNSHIP 3 SOUTH, RANGE 4 WEST, SAN BERNARDINO MERIDIAN, IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, SHOWN AS PARCEL 11 ON MAP FILED IN BOOK 110 PAGES 30 TO 40 INCLUSIVE, OF RECORDS OF SURVEY, IN THE OFFICE OF THE COUNTY RECORDER, OF SAID COUNTY, DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHERLY TERMINUS OF THAT CERTAIN COURSE SHOWN AS HAVING A BEARING AND DISTANCE OF NORTH 30°06'59" WEST 670.29 FEET IN THE EASTERLY BOUNDARY OF SAID PARCEL 11, SAID SOUTHERLY TERMINUS ALSO BEING A POINT ON THE NORTHERLY LINE OF PARCEL MAP NO. 8698, AS PER MAP FILED IN BOOK 37 PAGE 90, OF PARCEL MAPS IN THE OFFICE OF SAID RECORDER; THENCE ALONG SAID NORTHERLY LINE SOUTH 89°53'52" WEST 117.66 FEET; THENCE LEAVING SAID NORTHERLY LINE NORTH 30°09'25" WEST 124.78 FEET TO A LINE PARALLEL WITH 108.00 FEET NORTHERLY, MEASURED AT RIGHT ANGLES, FROM SAID NORTHERLY LINE; THENCE ALONG SAID PARALLEL LINE SOUTH 89°53'52" WEST 1955.75 FEET TO THE EASTERLY BOUNDARY OF CALIFORNIA STATE ROUTE 215, AS SHOWN ON CALIFORNIA DEPARTMENT OF TRANSPORTATION MONUMENTATION MAP 45680 ON FILE IN THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION; THENCE ALONG SAID EASTERLY BOUNDARY NORTH 19°22'43" WEST 4259.32 FEET; THENCE LEAVING SAID EASTERLY BOUNDARY NORTH 77°53'09" EAST 120.23 FEET; THENCE SOUTH 17°20'02" EAST 24.72 FEET; THENCE NORTH 75°40'21" EAST 81.83 FEET; THENCE SOUTH 45°37'50" EAST 766.67 FEET; THENCE NORTH 52°14'06" EAST 614.61 FEET TO THE NORTHWESTERLY TERMINUS OF THAT CERTAIN COURSE SHOWN AS HAVING A BEARING AND DISTANCE OF NORTH 30°07'27" WEST 3507.80 FEET IN THE BOUNDARY OF THE "MARCH AIR RESERVE BASE" AS SHOWN ON MAP FILED IN BOOK 124 PAGES 69 TO 81 INCLUSIVE OF SAID RECORDS OF SURVEY; THENCE ALONG SAID LAST MENTIONED BOUNDARY AS FOLLOWS: SOUTH 30°07'25" EAST 3507.87 FEET; THENCE SOUTH 49°46'59" WEST 73.50 FEET; THENCE SOUTH 39°57'49" EAST 421.43 FEET; THENCE SOUTH 30°06'29" EAST 670.22 FEET TO THE POINT OF BEGINNING.

APPENDIX B

Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table

General Plan Goal/Policy		Consistency
Land Use		
Goal 1	<i>Land Use Plan provides for a balanced mix of land uses that contribute to the regional setting, can capitalize on the assets of the Planning Area, while insuring compatibility throughout the Planning Area and with regional plans.</i>	The project provides for logistics/industrial uses. These uses are compatible with the General Plan and the adjacent Perris Valley Commerce Center Specific Plan. The project will include a General Plan Amendment adding a Specific Plan Overlay designation to the Site. Consistent with the Reuse Plan and General Plan, the project will continue to improve the balance of population and employment in the project vicinity, providing an opportunity for residents to work locally, rather than commute to surrounding areas throughout the region.
Policy 1.1	<i>Provide for a mix of land uses which implement the Base Master Reuse Plan for March AFB; offer a variety of employment opportunities; and capitalizes, enhances and expands upon existing physical and economic assets of the Planning Area.</i>	See response to Land Use Goal 1.
Policy 1.2	<i>Develop and maintain a system of land use designations and zoning districts which will provide locations for commercial, business park, manufacturing, aviation, public, and open space uses, and which actuates compatible and synergistic land uses.</i>	See response to Land Use Goal 1.

APPENDIX B

Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table

General Plan Goal/Policy		Consistency
Land Use		
<i>Policy 1.3</i>	<i>Provide for patterns of land use which can be supported by existing and planned circulation, public facilities, and infrastructure system improvements in a manner that will preserve the March JPA’s fiscal capacity.</i>	<p>The project is consistent with the approved General Plan circulation plan. The project will extend Van Buren Boulevard south of the March Air Field Museum. The General Plan designates the Van Buren Boulevard extension as a Major Arterial. This segment of Van Buren Boulevard will be designed as a Modified Secondary Highway, which while providing two traffic lanes, provides 97-feet of right-of-way instead of 100-feet of right-of-way the General Plan assigned to a Major Arterial. An additional 20-foot separation between the edge of sidewalk and screen walls associated with the Veterans Park Specific Plan.</p> <p>In addition, the project EIR will evaluate the utility providers’ ability to serve the project. Any significant impacts to public services or utilities will be mitigated through the environmental review process and prior to development.</p>
<i>Policy 1.4</i>	<i>Use specific and/or master plan processes for the coordinated development of large properties to ensure cohesive, comprehensive development.</i>	<p>The property does not currently have a zoning designation. The General Plan land use is Aviation, which would allow for a variety of uses including hangars, aviation support services, air cargo storage, fixed based operations, and aviation operations services. The Site does not have direct access to flying facilities. As such, industrial facilities described in the Specific Plan will allow for a mix of logistics center uses that could support e-commerce, wholesale, storage, distribution, manufacturing and/or assembly centers. It is anticipated that these uses would support airport-related businesses in the future.</p>
<i>Policy 1.5</i>	<i>Provide for a variety of industrial uses, including heavy manufacturing, light manufacturing, warehousing and distribution, transportation - related, and research and development.</i>	<p>The project will provide a mix of logistics center uses that could support e-commerce, wholesale, storage, distribution, manufacturing and/or assembly centers.</p>
<i>Policy 1.6</i>	<i>Locate and group commercial and industrial uses which are oriented toward regional service/market areas to promote utilization of regional transportation facilities and development-supporting infrastructure.</i>	<p>See response to Land Use Goal 1.</p>

APPENDIX B

Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table

General Plan Goal/Policy		Consistency
Land Use		
<i>Policy 1.9</i>	<i>Plan for compatible land uses within the aircraft noise impact contours depicted in the Air Installation Compatible Use Zones (AICUZ) Report for the airfield use.</i>	The project does not include specific aviation-related facilities, however, the development concept could support airport related businesses. Building height and uses described within the Specific Plan are consistent with the AICUZ guidelines and requirements.
Goal 2	<i>Locate land uses to minimize land use conflict or creating competing land uses, and achieve maximum land use compatibility while improving or maintaining the desired integrity of the Planning Area and subregion.</i>	The project provides the same types of land uses as designated in the General Plan. These land uses are compatible with the surrounding land uses. Incompatible or competing land uses will not be allowed in the project area.
<i>Policy 2.1</i>	<i>Avoid conflicts and incompatibilities between land uses through the use of landscaped setbacks and buffers, site design, site orientation, architectural features, walls or fences, density/intensity reductions, reduced hours of operation for commercial and industrial uses, shielding of lighting, and the like.</i>	The Specific Plan Design Guidelines provide the architectural, signage, walls and fences, lighting, and landscaping standards to help alleviate any land use incompatibilities with the surrounding areas.
<i>Policy 2.3</i>	<i>Support land uses that provide a balanced land use pattern of the Planning Area, and discourage land uses that conflict or compete with the services and/or plans of adjoining jurisdictions.</i>	See response to Land Use Goal 2.
<i>Policy 2.4</i>	<i>Protect the interests of, and existing commitments to adjacent residents, property owners, and local jurisdictions in planning land uses.</i>	See response to Land Use Goal 2.

APPENDIX B

Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table

General Plan Goal/Policy		Consistency
Land Use		
Goal 3	<i>Manage growth and development to avoid adverse environmental and fiscal effects.</i>	Development of the project is bound by the terms and conditions of the Development Agreement between the Master Developer and the March JPA. The project will be required to implement the applicable infrastructure and services per the Development Agreement. The Development Agreement accommodates a number of financing strategies to fund public improvements and minimize fiscal impacts.
Policy 3.1	<i>Manage growth so that its rate does not exceed the ability of March JP A or service districts to provide for an acceptable level of public facilities and services.</i>	The project EIR will evaluate the utility providers’ ability to serve the project. Any significant impacts to public services or utilities will be mitigated through the environmental review process and prior to development.
Goal 4	<i>Develop an identity and foster quality development within the Planning Area.</i>	The Specific Plan Design Guidelines will provide the architectural, signage, parking, and landscaping standards to achieve the goals of both project identity and quality development.
Policy 4.1	<i>Develop and maintain a land use plan for the Planning Area which proposes compatible land uses to create distinct, identifiable historic, commercial, industrial, public, and aviation areas.</i>	See Response to Land Use Goal 1.
Policy 4.4	<i>Develop a distinctive community identity for commercial , business park and industrial developments that reflect the character and atmosphere of March JPA Planning Area through the use of good planning and design principals , and sound development practices which serve as guidelines for building materials, colors, site design and orientation, and landscaping.</i>	See responses to Land Use Goals 1 and 4.

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Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table

General Plan Goal/Policy		Consistency
Land Use		
<i>Policy 4.7</i>	<i>Develop and enhance the economic climate and create a balanced business community to serve the work force, commerce and industry of the region.</i>	See response to Land Use Goal 1.
Goal 5	<i>Maximize and enhance the tax base and generation of jobs through new, reuse and joint use opportunities.</i>	The project will expand on the large employment center concept started by the adjacent Meridian Specific Plan and Perris Valley Commerce Center Specific Plan. As such, it will provide a substantial enhancement to the tax base.
<i>Policy 5.1</i>	<i>Support the development and establishment of new employment centers and economic development activities that contribute to an improved tax base.</i>	See response to Land Use Goal 5.
<i>Policy 5.2</i>	<i>Encourage and facilitate the creation of public/private partnerships that will invest in, and further the implementation of the March AFB Master Reuse Plan.</i>	Riverside Inland Development, LLC, has entered into a Memorandum of Understanding with March JPA related to development of the property within the Specific Plan area. The applicant, Riverside Inland Development, LLC, has assumed the responsibilities and obligations of the Master Developer for the remaining areas of the former March Air Force Base (which includes this project area) in a Disposition and Development Agreement which will be processed concurrently with the Veterans Industrial Park 215 Specific Plan.
<i>Policy 5.5</i>	<i>Encourage the development of commercial, business park and industrial centers to expand the employment and fiscal base of the March JPA Planning Area and the western Riverside County Subregion.</i>	See response to Land Use Goal 1.

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Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table

General Plan Goal/Policy		Consistency
Land Use		
Goal 6	<i>Support the continued Military Mission of March Air Reserve Base, and preservation of the airfield from incompatible land use encroachment.</i>	The project is designed to incorporate appropriate uses as defined in the Air Installation Compatible Use Zone (AICUZ) Study, Airport Layout Plan, Department of Defense Instructions and Air Force Instructions.
Policy 6.3	<i>Ensure that plans and development do not interfere, conflict or degrade the military mission of March ARB.</i>	See response to Land Use Goal 6.
Policy 6.4	<i>Ensure that plans and development do not conflict with the long-term needs of the Air Force Reserve in terms of encroachment, noise, accident zone, constraints, etc.</i>	See response to Land Use Goal 6.
Policy 6.5	<i>Ensure that plans and development conform to the draft Comprehensive Land Use Plan for March AFB/March Inland Port.</i>	See response to Land Use Goal 6.
Policy 6.8	<i>Ensure that land uses adhere to both military and civilian Part 77 conical surface criteria, relative to height restrictions.</i>	The project will obtain concurrence letters from the FAA stating, “no hazard to air navigation” for the proposed buildings within the development.
Goal 7	<i>Maximize the development potential as a regional Intermodal Transportation facility to support both passenger and freight related air services</i>	The Veterans Industrial Park 215 Specific Plan includes industrial land uses compatible with the adjacent aviation uses through compliance with runway height limitations, provision of security fencing, and provision of warehousing, and logistics, which could support aviation-related business.
Policy 7.6	<i>Plan for compatible land uses within the aviation area.</i>	See response to Land Use Goal 7.

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Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table

General Plan Goal/Policy		Consistency
Land Use		
Goal 8	<i>Preserve the natural beauty, minimize degradation of the March JPA Planning Area, and provide enhancement of environmental resources and scenic vistas.</i>	There are no known sensitive environmental areas near the site. A jurisdictional drainage channel is present, crossing the project site; impacts to this drainage will be addressed through a permitting process with appropriate agencies. Design features and mitigation measures to minimize the impacts to potential sensitive land uses from the project will be evaluated during the environmental review process.
Policy 8.1	<i>Where practical, revegetate graded area with native plants compatible to the area to prevent erosion.</i>	The project will comply with the requirements of the California Construction General Permit (SWRCB Orders No. 2009-009-DWQ as amended by Order 2010-0014-DWQ and Order 2012-006-DWQ) and employ Best Management Practices (BMPs) to minimize erosion for graded areas.
Goal 10	<i>Avoid undue burdening of infrastructure, public facilities, and services by requiring new development to contribute to the improvement and development of the March JPA Planning Area.</i>	See response to Land Use Goal 3.
Policy 10.1	<i>Require new construction to pay its "fair share" of the cost of providing adequate public services, infrastructure, and facilities for the development.</i>	See response to Land Use Goal 3. The project sponsors will finance the public service extensions to water and sewer lines to serve the project, as well as extending Van Buren Boulevard adjacent to the site. In addition, the project EIR will evaluate traffic and transportation impacts. Any significant impacts to traffic will be mitigated through the environmental review process and prior to development. Further, the project will pay its fair share of the cost of providing services and infrastructure through payment of the development impact fees assessed by the JPA.
Policy 10.2	<i>Require new construction to provide adequate infrastructure to serve the development (i.e., curbs and gutters, sidewalks, street lights, water service, sewer service or septic systems, etc.) prior to initiation of use.</i>	See response to Land Use Goal 3.

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Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table

General Plan Goal/Policy		Consistency
Land Use		
<i>Policy 10.3</i>	<i>Locate commercial and industrial development in areas where street rights-of-way and capacity are available, as well as sufficient infrastructure and public services.</i>	See response to Land Use Goal 3.
Goal 12	<i>Ensure, plan, and provide adequate infrastructure for all facility reuse and new development, including but not limited to, integrated infrastructure planning, financing and implementation.</i>	See response to Land Use Goal 3.
<i>Policy 12.3</i>	<i>Require new development projects to provide for the extension of infrastructure to serve the development, including over-sizing facilities for future needs.</i>	See responses to Land Use Policy 1.3 and Goal 3.
Goal 13	<i>Secure adequate water supply system capable of meeting normal and emergency demands for existing and future land uses.</i>	The project EIR will evaluate the utility providers’ ability to serve the project. Any significant impacts to public services or utilities will be mitigated through the environmental review process and prior to development. A Water Supply Assessment will be prepared as part of project entitlement by Western Municipal Water District to evaluate the required water supply for the subject logistics project.
<i>Policy 13.2</i>	<i>Enhance local groundwater supplies through development designs which promote an on-site recharge and minimize impermeable ground coverage with landscaped areas, open space or recreation areas.</i>	See response to Land Use Goal 17. Note that due to the close proximity to the airport runways the project must minimize standing water while addressing water quality requirements for the site. On-site bio-retention basins will treat on-site runoff while minimizing standing water.

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Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table

General Plan Goal/Policy		Consistency
Land Use		
Goal 14	<i>Establish, extend, maintain and finance a safe and efficient wastewater collection, treatment and disposal system, which maximizes treatment and water recharges, minimizes water use, and prevents groundwater contamination.</i>	The project will provide the necessary conveyance facilities to achieve this goal. The project EIR will evaluate infrastructure requirements, including conveyance and treatment of wastewater.
Policy 14.1	<i>Require all development to adequately collect, treat, and dispose of wastewater in accordance with the Santa Ana Regional Water Quality Control Board requirements.</i>	The project will comply with the March JPA’s NPDES New Development and Redevelopment Guidelines for projects Under the March Joint Powers Authority and the Santa Ana Regional Water Quality Control Board Order No. R8-2002-0011.
Policy 14.2	<i>Require connection to the sewer system for any development occurring on land formerly part of March AFB.</i>	See response to Goal 14 and Land Use Policy 14.1. As shown in Figure 3-6 of the Specific Plan, the project will extend sewer lines to service the project, connecting to the existing sewer lines adjacent to the Specific Plan area.
Policy 14.3	<i>Encourage reuse of reclaimed and treated non-potable water for irrigation and maintenance of recreation areas, landscaping and /open space preservation.</i>	Reclaimed water is not available for use within the project.
Goal 16	<i>Adequate supplies of natural gas and electricity from utility purveyors and the availability of communications services shall be provided within the March JPA Planning Area.</i>	The project EIR will evaluate the utility providers’ ability to serve the project. Any significant impacts to public services or utilities will be mitigated through the environmental review process and prior to development.
Policy 16.1	<i>Where feasible, require new development to underground on-site telecommunication connections.</i>	The project will underground on-site telecommunication connections.

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General Plan Goal/Policy		Consistency
Land Use		
Goal 17	<i>Adequate flood control facilities shall be provided prior to, and concurrent with, development in order to protect the lives and property within the March JPA Planning Area.</i>	The project will comply with the March JPA's NPDES New Development and Redevelopment Guidelines for projects Under the March Joint Powers Authority and the Santa Ana Regional Water Quality Control Board Order No. R8-2002-0011. A hydrology study and water quality management plan (WQMP) will be prepared for the project. It is not presumed that project implementation would have a substantial impact on water quality standards or waste discharge requirements. The project will provide for a drainage plan to convey on-site flows to the existing drainage areas downstream of the project. A regional drainage solution is in process to convey off-site flows around the project site to the existing drainage facilities downstream. Any significant impacts to hydrology and water quality will be mitigated through the environmental review process and prior to development.
Policy 17.1	<i>Provide for the adequate drainage of storm runoff to protect the lives and property within the Planning Area.</i>	See response to Land Use Goal 17.
Policy 17.2	<i>Monitor and maintain drainage and flood control facilities to ensure adequate capacity to support the land use plan.</i>	See response to Land Use Goal 17.
Policy 17.3	<i>Require new development to construct new or upgrade existing drainage facilities to accommodate the additional storm runoff caused by the development.</i>	The project's hydrology study evaluates the system required to capture and convey on-site runoff. Site runoff will be captured and detained in two on-site basins, one located in each of the two planning areas within the Specific Plan area. Water will be detained, treated, and released at a rate consistent with the existing condition.

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Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table

General Plan Goal/Policy		Consistency
Transportation		
Goal 1	<i>Establish and provide for a comprehensive transportation system that captures the assets and opportunities of the planning area, existing transportation facilities, and planned transportation facilities for the future growth and development of the planning area and sub-region.</i>	The project is located to the east of the Meridian Specific Plan area. The existing infrastructure for the Meridian development will be extended to the east to support the project, including extension of Van Buren Boulevard from its existing terminus to the extension of Western Way on the Specific Plan’s southern edge. This road extension is consistent with the planned roadway network in the General Plan Circulation Element.
Policy 1.1	<i>Plan for a mix of transportation modes aimed at effective use of resources, both physical infrastructure and natural energy resources.</i>	See responses to Transportation Goals 1 and 2.
Policy 1.2	<i>Design transportation improvements which are compatible with the natural environment. Xeriscape and drought tolerant landscaping techniques should be used for all parkway and median plantings. Where feasible non-potable water should be used for irrigation purposes.</i>	See responses to Land Use Policy 14.3 and Resource Management Policy 1.5. The Specific Plan’s landscape plan illustrates the proposed landscaping within the Specific Plan Area and in the proposed roadways supporting the project. The plant palette outlined in Table 4-1 of the Specific Plan is comprised of drought tolerant landscape materials.
Policy 1.4	<i>Roadway system shall inter-relate with the components of the multi-faceted transportation system that will assist with the synergistic value of each element's effectiveness (i.e., bike lockers at the Metrolink station, with bike lanes emanating there from).</i>	See responses to Transportation Goals 1 and 2. No bikeways are identified in the General Plan as part of the extension of Van Buren Boulevard east of I-215. The proposed Van Buren extension includes an on-street bicycle lane as part of the project improvements, and on-site bicycle parking is provided.

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Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table

General Plan Goal/Policy		Consistency
Transportation		
Goal 2	<i>Build and maintain a transportation system which capitalizes on the multi-faceted elements of transportation planning and systems, designed to meet the needs of the planning area, while minimizing negative effects on air quality, the environment and adjacent land uses and jurisdictions.</i>	No bikeways are identified in the General Plan as part of the extension of Van Buren Boulevard east of I-215. The proposed Van Buren extension includes an on-street bicycle lane as part of the project improvements, and on-site bicycle parking is provided.
Policy 2.6	<i>FAA Standards, military AICUZ, and appropriate Comprehensive Land Use Plan for March Airfield shall be upheld and supported to encourage and realize a safe environment in and around the aviation field.</i>	FAA standards and AICUZ requirements, as well as Department of Defense Instructions and Air Force Instructions, will be complied with. The project’s land use plan and development regulations include height limitations associated with the 35-foot building restriction line and the 7:1 height requirements. In addition, the proposed uses comply with the Zone B2 occupancy and use restrictions. Security fencing will be provided adjacent to the runway. In addition, no standing water exceeding the airport’s 48 hour criteria will be allowed as part of the project’s drainage plans.
Policy 2.7	<i>On-street parking shall be de-emphasized throughout the planning area to permit maximum capacity of roadways to be actuated by vehicular and bicycle transportation modes.</i>	The project will provide adequate off-street parking to limit the potential for on-street parking. This will allow for more capacity on the roadways for sidewalks and landscaping.
Policy 2.8	<i>Street improvements shall be designed in a comprehensive manner to include parkway facilities, pedestrian walkways, commuter bike lanes, signing, lighting noise and air quality factors, as applicable.</i>	The project will comply with the roadway standards within the Circulation Plan associated with this Specific Plan and its associated Design Guidelines. The Specific Plan specifies the cross-sections for public roadways which include sidewalks, medians, and landscaping. The Design Guidelines provide the architectural, signage, parking, and landscaping standards to achieve the goals of both project identity and quality development. For the Van Buren Boulevard extension to the east of I-215, the General Plan does not include on-street bike lanes.

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General Plan Goal/Policy		Consistency
Transportation		
Goal 3	<i>Develop a transportation system that is safe, convenient, efficient and provides adequate capacity to meet local and regional demands.</i>	This project will construct an internal driveway/private drive network and provide for the extension of Van Buren Boulevard from its existing terminus to a planned extension of Western Way on the Specific Plan's southern edge, based on future demand and the General Plan circulation element. Transportation improvements will be constructed and phased as determined by the project EIR's traffic analysis. The project EIR will evaluate traffic and transportation impacts. Any significant impacts to traffic will be mitigated through the environmental review process and prior to development.
Policy 3.1	<i>Follow standards for transportation element roadways in designing and constructing street improvements.</i>	See response to Transportation Policy 2.8.
Goal 4	<i>Provide a balanced transportation system that ensures the safe and efficient movement of people and goods throughout the planning area, while minimizing the use of land for transportation facilities.</i>	project internal drives and adjacent public roadways (Van Buren Boulevard and Western Way extensions) will be sized to accommodate projected future traffic in an efficient manner.
Policy 4.2	<i>All streets shall be constructed in accordance with planning area's standard street classifications. Modifications within the Northeast Planning Subarea to the standard street classifications may be allowed on a limited basis to preserve the integrity of the area and facilities.</i>	See response to Transportation Policy 2.8.

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General Plan Goal/Policy		Consistency
Transportation		
Policy 4.10	<i>Work with the City of Perris to plan for an arterial roadway on the east frontage of I-215 between Van Buren Boulevard and Oleander Avenue, in order to preserve future options for developing a passenger or air cargo terminal on the west side of the runway.</i>	The proposed extension of Van Buren Boulevard on the eastern edge of I-215, connecting with a proposed extension of Western Way in the City of Perris, implements this requirement.
Goal 6	<i>Establish vehicular access control policies in order to maintain and insure the effectiveness and capacity of arterial roadways.</i>	project internal roadways will be designed in accordance with the “County Road Improvement Standards and Specifications,” published by the County of Riverside, and take into account additional landscaping requirements established in the Riverside County Integrated Plan County standards, and implement appropriate intersection and driveways intervals on arterial roadways.
Policy 6.2	<i>Access to an arterial road shall be limited to one point for every 300 feet of frontage or one point for parcels with less than 300 feet of frontage.</i>	The Specific Plan area includes approximately 5,500 linear feet of frontage on the proposed extension of Van Buren Boulevard, allowing for approximately 18 access points. The Specific Plan land use includes six points of access and is thus consistent with this requirement.
Goal 7	<i>Facilitate and develop transportation demand management and transportation systems management programs, and use of alternate transportation modes.</i>	Transportation Demand Management (TDM) strategies will be implemented to shift trips outside the standard commuting hours and/or to non-“drive alone” modes of travel. This is accomplished through various employer-initiated measures, such as flexible working hours, encouragement of carpooling, and facilitating access for non-motorized (i.e., bicycling or walking) modes of travel.
Policy 7.5	<i>Provide a system of bicycle facilities (paths, lanes and routes) in conjunction with circulation system roadway improvements.</i>	No bikeways are identified in the General Plan as part of the extension of Van Buren Boulevard east of I-215. The proposed Van Buren extension includes an on-street bicycle lane as part of the project improvements, and on-site bicycle parking is provided.

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General Plan Goal/Policy		Consistency
Transportation		
Goal 8	<i>Adequate, affordable, equitably distributed and energy efficient public and mass transit services which promote the mobility to, from, and within the planning area shall be provided.</i>	See response to Transportation Goal 2. The local transit system of bus stops and bus shelters will be approved by the Riverside Transit Agency (RTA) as appropriate along the proposed extension of Van Buren Boulevard and Western Way.
Policy 8.1	<i>Evaluate transportation alternatives with project design, development and implementation.</i>	See responses to Transportation Goals 1 and 2.
Goal 9	<i>Develop measures which will reduce the number of vehicle-miles traveled during peak travel periods.</i>	See response to Land Use Goal 1. The project will provide a large employment base, which will provide an opportunity for residents in the vicinity to work locally, rather than commute to Los Angeles or Orange Counties. This improved Jobs/housing balance will help reduce vehicle miles traveled.
Policy 9.1	<i>Provide incentives to employers who encourage carpooling and vanpooling for employees.</i>	See response to Transportation Goal 7.
Policy 9.2	<i>Provide preferential parking for carpools and vanpools, where appropriate.</i>	The Specific Plan development regulations require a minimum of 5% carpool/vanpool parking spaces within the Specific Plan area.
Goal 10	<i>Regulate the travel of trucks on March JPA Planning Area streets.</i>	The project is designed to accommodate truck traffic. Western Way is a designated truck route in the City of Perris, and Van Buren Boulevard is a truck route as designated in the General Plan.

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General Plan Goal/Policy		Consistency
Transportation		
Policy 10.1	<i>Establish a truck route system which designates truck and commercial vehicle routes and provides adequately sized and designed roadways to meet the needs of trucks and commercial vehicles. This will eliminate truck and commercial vehicle traffic through inappropriate areas of the March JPA Planning Area.</i>	See response to Transportation Goal 10.
Policy 10.2	<i>Clearly sign designated truck routes and identify maximum weight limitations on these routes.</i>	See response to Transportation Goal 10. This requirement would be implemented as part of construction plans for the public roadways developed as part of the project (Van Buren Boulevard and Western Way).
Goal 11	<i>Adequate off-street parking for all land uses shall be provided which requires adequate on-site parking to prevent spill over on the adjacent street system.</i>	The project will provide adequate parking to limit the potential for parking spillover on to streets. Parking will be provided in accordance with the Development Regulations established by the Specific Plan. Parking ratios have been provided based on square footage. Parking ratios for car/vanpools, bicycle parking, and charging stations have also been provided in the Specific Plan development regulations.
Policy 11.1	<i>Provide for adequate parking facilities for all uses.</i>	See response to Transportation Goal 11.
Policy 11.4	<i>Require all new development to provide adequate off-street parking based on expected parking needs</i>	See response to Transportation Goal 11.
Policy 11.5	<i>Provide adequate loading areas within off-street parking areas for all commercial and manufacturing land uses.</i>	The project will provide adequate loading areas associated with the proposed logistics/industrial buildings.

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General Plan Goal/Policy		Consistency
Transportation		
Goal 12	<i>Plan for and seek to establish and area-wide system of bicycling trails, with linkages within the planning area and with adjacent jurisdictions, and in compliance with sub-regional plans.</i>	No bikeways are identified in the General Plan as part of the extension of Van Buren Boulevard east of I-215. The proposed Van Buren extension includes an on-street bicycle lane as part of the project improvements, and on-site bicycle parking is provided.
Policy 12.7	<i>Require sidewalks on both sides of all streets. The March JPA encourages alternate designs including parkways and meandering and enhanced paving.</i>	As shown in the street cross sections of the Specific Plan’s Circulation Plan, sidewalks are provided on the project-frontage of Van Buren Boulevard, and right of way accommodating sidewalks is provided on both Van Buren Boulevard and Western Way extensions.
Goal 13	<i>Promote, preserve, and protect the joint use of the aviation field by the Air Force Reserves and civilian aviation.</i>	The project will not impact the use of the air field in any way.
Policy 13.6	<i>Protect flight paths from inappropriate development encroachment.</i>	The project is designed to incorporate appropriate uses as defined in the Air Installation Compatible Use Zone (AICUZ) Study, Airport Layout Plan, Department of Defense Instructions and Air Force Instructions
Policy 13.8	<i>Adhere to approved airport layout plans approved by the Joint Powers Commissions and recognized by the FAA.</i>	See response to Policy 13.6 above.
Goal 15	<i>In accordance with state and federal law, promote and provide mobility for the disabled.</i>	Development plans shall take into account the accessibility requirements of the Americans with Disability Act (ADA).
Policy 15.1	<i>Require that all development comply with the requirements of the state and federal law for the disabled. Requirements may include ramps at street corners, access to public buildings, traffic signal timing and the like.</i>	See response to Transportation Goal 15.

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General Plan Goal/Policy		Consistency
Noise		
Goal 1	<i>Ensure that land uses are protected from excessive and unwanted noise.</i>	project development shall be consistent with the land use and noise limitations established in the AICUZ study and the Riverside County Airport Land Use Plan and MJPA Development Code. Industrial uses are consistent with the noise limitations of the AICUZ study.
Policy 1.1	<i>Establish acceptable limits of noise for various land uses throughout the March JPA Planning Area. Future development that could increase ambient noise levels shall be required to mitigate the anticipated noise increase, to the extent possible.</i>	The project EIR will evaluate noise impacts. The Specific Plan area is located adjacent to the airport runway, an inherently noisy environment. Any significant impacts from project noise will be mitigated through the environmental review process and prior to development. Industrial uses are consistent with the noise limitations of the AICUZ study.
Policy 1.3	<i>Encourage good acoustical design in new construction.</i>	See response to Noise Policy 1.1.
Goal 2	<i>Minimize incompatible noise level exposures throughout the Planning Area, and where possible, mitigate the effect of noise incompatibilities to provide a safe and healthy environment.</i>	No sensitive uses are located in proximity to the proposed Specific Plan area. Design features and mitigation measures to minimize noise impacts from the project will be evaluated during the environmental review process. Incompatible land uses will not be allowed in the project area.
Policy 2.4	<i>March JPA shall evaluate noise sensitivity and noise generation when considering land use projects and transportation improvement projects, and where appropriate mitigation measures shall be employed.</i>	See response to Noise Goal 2.

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Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table

General Plan Goal/Policy		Consistency
Noise		
Goal 3	<i>Work toward the reduction of noise impacts from vehicular traffic, and aviation and rail operations.</i>	See response to Noise Goal 2.
Policy 3.4	<i>Where appropriate, noise mitigation measures shall be incorporated in the design and approval of development property located adjacent to aviation and rail facilities.</i>	Noise mitigation, if required, will be incorporated into the project based on the noise studies prepared as part of the project’s Environmental Impact Report.
Policy 3.7	<i>Limit trucking operations to appropriate routes, times and speeds.</i>	The project is designed to accommodate truck traffic. Western Way is a designated truck route in the City of Perris, and Van Buren Boulevard is a truck route as designated in the General Plan. Standard speeds would apply on public truck routes.
Policy 3.8	<i>Appropriate muffling systems for construction equipment and operations shall be required, as necessary.</i>	The project EIR will evaluate noise impacts associated with construction. Any significant impacts from project construction noise will be mitigated through the environmental review process and prior to development.

General Plan Goal/Policy		Consistency
Air Quality		
Goal 1	<i>Promote alternative modes of travel.</i>	See response to Transportation Goal 2.
Policy 1.3	<i>Support trip-reduction programs, such as longer work days, shorter week work schedules.</i>	See response to Transportation Goal 7.

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General Plan Goal/Policy		Consistency
Air Quality		
Goal 2	<i>Reduce emissions associated with vehicle miles traveled by enhancing the jobs/housing balance of the subregion of western Riverside County.</i>	The project will provide a large employment base, which will provide an opportunity for residents in the vicinity to work locally, rather than commute to Los Angeles or Orange Counties. This improved Jobs/housing balance will help reduce vehicle miles traveled.
Policy 2.1	<i>Create an employment center within the housing rich environment of the subregion.</i>	See responses to Air Quality Goal 1 and Transportation Goal 9.
Goal 3	<i>Reduce air pollution through proper land use, transportation and energy use planning.</i>	See responses to Transportation Goals 2 and 7.
Policy 3.1	<i>Locate ancillary uses within business and employment centers to reduce the number of vehicle trips and lessen the vehicle miles traveled.</i>	The Specific Plan development regulations allow for ancillary office and retail uses in association with the proposed logistics uses. Inclusion of these ancillary uses will reduce the need for outside vehicular trips.
Policy 3.2	<i>Locate service uses and facilities in convenient proximity to employment and business center areas to encourage pedestrian or alternative transit to reduce the number of vehicle trips.</i>	See responses to Land Use Goal 1 and Transportation Goal 9.
Policy 3.3	<i>Develop a bike lane network that will link the bike lanes to residential areas adjacent to the Planning Area and Metrolink Stations to encourage non-motorized travel within the planning area.</i>	No bikeways are identified in the General Plan as part of the extension of Van Buren Boulevard east of I-215. No bikeways are planned as part of the project improvements, although on-site bicycle parking is provided.
Policy 3.4	<i>Encourage ride share programs.</i>	See response to Transportation Goal 7.

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General Plan Goal/Policy		Consistency
Air Quality		
Policy 3.5	<i>Parking facilities shall be designed to safely accommodate and support alternative modes of transportation and preferential location of alternative fuel vehicles and mass transit services.</i>	See response to Transportation Goal 2. In addition, the project will include charging stations for electric vehicles within the parking areas of the plan consistent with CAL Green standards.
Goal 5	<i>Maximize the effectiveness of air quality control programs through coordination with other governmental entities.</i>	The project will comply with South Coast Air Quality Management District rules and regulations.
Policy 5.5	<i>Review development projects to determine the potential air quality impacts and provide appropriate mitigation, where necessary.</i>	The project EIR will evaluate air quality impacts. Any significant impacts to air quality from the project will be mitigated through the environmental review process and prior to development.
Goal 6	<i>Reduce emissions associated with vehicle/engine use.</i>	See responses to Land Use Goal 1 and Transportation Goal 9.
Policy 6.3	<i>Encourage diversion of peak hour truck traffic, whenever feasible, to off-peak periods to reduce roadway congestion and associated emissions.</i>	See response to Transportation Goal 10.
Policy 6.5	<i>Encourage trucks operating within March JPA Planning Area to maintain safety equipment and operate at safe speeds so as to reduce the potential for accidents which create congestion and related emissions.</i>	The project EIR will evaluate air quality impacts. Any significant impacts to air quality from the project will be mitigated through the environmental review process and prior to development.

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Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table

General Plan Goal/Policy		Consistency
Air Quality		
Goal 7	<i>Reduce emissions associated with energy consumption.</i>	Buildings shall be designed to reduce energy usage through various measures (such as energy efficient lighting and glazing, using lighter colored roofing materials, orienting buildings north and increasing wall insulation above Title 24 requirements, etc.) The project’s EIR will analyze the impacts associated with energy consumption and proposed mitigation measures to reduce impacts.
Policy 7.1	<i>Support the use of energy-efficient equipment and design in the March JPA Planning Area for facilities and infrastructure.</i>	See response to Air Quality Goal 7.
Policy 7.3	<i>Support passive solar design in new construction.</i>	See response to Air Quality Goal 7.
Policy 7.4	<i>Support recycling programs which reduce emissions associated with manufacturing and waste disposal.</i>	Recycling of materials will be a component of the waste management program of uses within the Specific Plan area.
Policy 7.5	<i>Support drought-resistant vegetation in landscaping areas to reduce energy needed to pump water.</i>	The specific Plan includes a landscape plan and plant palette for development within the Specific Plan area. The project’s plant materials include predominantly drought tolerant vegetation.
Goal 8	<i>Reduce air pollution emissions and impacts through siting and building design.</i>	See response to Air Quality Goal 7. Design features and mitigation measures to minimize the impacts to air quality from the project will be evaluated during the environmental review process.
Policy 8.1	<i>Support the use of low polluting construction materials and coatings.</i>	The project EIR will evaluate air quality impacts from construction. Any significant impacts to air quality from the project will be mitigated through the environmental review process and prior to development.

APPENDIX B

Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table

General Plan Goal/Policy		Consistency
Air Quality		
Goal 9	<i>Reduce fugitive dust and particulate matter emissions.</i>	Trucks hauling dirt, sand, gravel or soil are to be covered or should maintain at least two feet of freeboard in accordance with Section 23114 of the California Vehicle Code. Where feasible, construction access roads to the main roads should be paved to avoid dirt being carried on to the roadway or track-out devices should be installed. In addition, the project EIR will evaluate impacts associated with fugitive dust and particulate matter during construction and proposed mitigation measures to reduce or eliminate impacts.
Policy 9.1	<i>Require all feasible fugitive dust reduction techniques to be utilized during construction activities.</i>	See response to Air Quality Goal 9.

General Plan Goal/Policy		Consistency
Housing		
The General Plan does not allow for housing opportunities within the March JPA Planning Area due to incompatible uses with the airfield, the need to focus on the reestablishment of the numerous jobs lost due to base realignment, and the housing rich environment of Western Riverside County. The project maintains consistency with the General Plan’s absence of a residential land use designation within the Specific Plan area.		

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Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table

General Plan Goal/Policy		Consistency
Resource Management		
Goal 1	<i>Conserve and protect surface water, groundwater, and imported water resources.</i>	The project will be constructed to mitigate impacts to the existing drainage channel. Furthermore, the project EIR will evaluate impacts to hydrology and water supply from the project. Any significant impacts to hydrology or water supply from the project will be mitigated through the environmental review process and prior to development.
Policy 1.1	<i>Where possible, retain local drainage courses, channels and creeks in their natural condition.</i>	See response to Resource Management Goal 1. The on-site drainage is related to regional water flows across the site and is not a natural stream course. The proposed grading plan impacts this drainage to facilitate development of the site and to provide an alternative drainage conveyance plan for regional flows. Impacts will be mitigated in accordance with agency permitting requirements.
Policy 1.2	<i>Protect groundwater and surface water resources from depletion and sources of pollution.</i>	See responses to Land Use Goal 17 and Resource Management Goal 1.
Policy 1.4	<i>Require development to conserve water resources, including the use of water-efficient plumbing fixtures and irrigation systems.</i>	See response to Air Quality Goal 7.
Policy 1.5	<i>Conserve imported water by requiring water conservation techniques, water-conserving and recycling processes, drought-resistant landscaping, and reclaimed water for irrigation, when available and appropriate.</i>	The project will comply with the Specific Plan’s Design Guidelines. The Design Guidelines require the use of drought-resistant landscaping by the project. Reclaimed water will be used if available.
Policy 1.6	<i>Promote the use of drought tolerant landscaping in development, and encourage the use of reclaimed water for irrigation in parks, golf courses, and industrial uses, as well as for other urban uses, whenever feasible and where legally permitted.</i>	See response to Resource Management Policy 1.5. The Specific Plan includes a landscape plan and plant palette for development within the Specific Plan area. The project’s plant materials include predominantly drought tolerant vegetation. The project does not contain parks.

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Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table

General Plan Goal/Policy		Consistency
Resource Management		
Policy 1.8	<i>Assure that development projects comply with regulatory agency requirements, including federal, state and regional regulations.</i>	The project shall be required to obtain a Clean Water Act (CWA) Section 404 permit (regulated by the U.S Army Corps of Engineers), a Streambed Alteration Agreement (regulated by the California Department of Fish and Wildlife), a CWA Section 401 Water Quality Certification (regulated by the Regional Water Quality Control Board) to impact waters of the U.S/State.
Goal 2	<i>Control flooding to reduce major losses of life and property.</i>	See response to Land Use Goal 17.
Policy 2.3	<i>Ensure that development does not divert storm water run off onto adjacent properties, or cause alterations of natural drainage courses that cannot be adequately handled by flood control improvements installed coincident with the development.</i>	See response to Land Use Goal 17.
Policy 2.5	<i>To the greatest extent possible, require development to use master flood control facilities and limit use of interim drainage facilities or open channels.</i>	The drainage plan for the project will utilize the existing open channel as a downstream conveyance system as well as an open trapezoidal channel along the project’s easterly property line as an interim solution to a regional issue. See response to Land Use Goal 17.
Goal 3	<i>Conserve and protect significant land forms, important watershed areas, mineral resources and soil conditions.</i>	The project will not impact any significant landforms or mineral resources. The project EIR will evaluate impacts to hydrology, geology and soils from the project. Any significant impacts to hydrology, geology and soils from the project will be mitigated through the environmental review process and prior to development.

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Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table

General Plan Goal/Policy		Consistency
Resource Management		
Policy 3.1	<i>Conserve hillsides and rock outcroppings in the planning area through the use of master-planned developments which create a “Campus-like” setting, and encourage the creative siting of building areas as a means of retaining natural areas and open space.</i>	The project will be constructed in a flat area with no rock outcroppings and will not impact any of the area’s hillsides.
Policy 3.5	<i>Require and practice proper soil management techniques to reduce erosion, sedimentation and other soil-related problems.</i>	See response to Land Use Policy 8.1. A SWPPP and Erosion Control Plan will be required for the project to reduce erosion and other soil related problems. In addition, all grading will be reviewed by a soils consultant as part of grading permit preparation.
Policy 3.6	<i>Control erosion during and following construction through proper grading techniques, vegetation replanting, and the installation of proper drainage control improvements.</i>	See response to Land Use Policy 8.1 and Resource Management Policy 3.5
Policy 3.7	<i>Require erosion control measures such as binders, revegetation, slope covers, and other practices which reduce soil erosion due to wind and water.</i>	See response to Land Use Policy 8.1 and Resource Management Policy 3.5
Goal 4	<i>Conserve energy resources through use of available energy technology and conservation practices.</i>	See response to Air Quality Goal 7.
Policy 4.1	<i>Implement energy performance requirements established under the California Administration Code Title 24 Energy Conservation and Insulation Regulations.</i>	See response to Air Quality Goal 7.

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Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table

General Plan Goal/Policy		Consistency
Resource Management		
<i>Policy 4.2</i>	<i>Encourage innovative building, site design and orientation techniques which minimize energy use by taking advantage of sun/shade patterns, prevailing winds, landscaping, and building materials available to control energy usage.</i>	See response to Air Quality Goal 7.
<i>Policy 4.3</i>	<i>Encourage the use and development of alternative and innovative energy resources and energy conservation techniques, where practical.</i>	See response to Air Quality Goal 7.
Goal 5	<i>Conserve and protect significant stands of mature trees, native vegetation, and habitat within the planning area.</i>	The project EIR will evaluate impacts to biological resources from the project. Any significant impacts to biological resources from the project will be mitigated through the environmental review process and prior to development. The project would comply with the federal, state and local regulations regarding impacts to sensitive biological resources.
<i>Policy 5.1</i>	<i>Where practical, conserve important plant communities and habitats such as riparian areas, wetlands, significant tree stands, and species by using buffers, creative site planning, revegetation and open space easements/dedications.</i>	See responses to Resource Management Goals 1 and 5.
<i>Policy 5.2</i>	<i>Encourage the planting of native species of trees and other drought-tolerant vegetation.</i>	See response to Resource Management Policy 1.5.

APPENDIX B

Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table

General Plan Goal/Policy		Consistency
Resource Management		
Policy 5.4	<i>In areas that may contain important plant and animal communities, require development to prepare biological assessments identifying species types and locations and develop measures to preserve recognized sensitive species, as appropriate.</i>	See responses to Resource Management Policy 1.8 and Goal 5. The site is not located in an area with important plant and animal communities.
Policy 5.6	<i>Work with state, federal and local agencies in the preservation and/or mitigation of recognized sensitive vegetation and wildlife in March JPA Planning Area.</i>	See response to Resource Management Policy 1.8.
Goal 6	<i>Provide an effective and efficient waste management system for solid and hazardous wastes that is financially and environmentally responsible.</i>	The project shall comply with appropriate and applicable regulations and standards with respect to the management of solid and hazardous wastes.
Policy 6.4	<i>Coordinate with regulatory agencies in assuring that future development handles and disposes of hazardous materials in compliance with applicable regulations.</i>	See response to Safety Risk/Management Goal 4.
Goal 7	<i>Promote cultural awareness through preservation of the planning area’s historic, archaeological and paleontological resources.</i>	The project is not anticipated to impact significant historic, archaeological or paleontological resources. The project EIR will comply with the requirements of AB52 and SB18 related to Tribal consultation.

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Veteran's Industrial Park 215 Specific Plan - General Plan Consistency Table

General Plan Goal/Policy		Consistency
Resource Management		
Policy 7.5	<i>Require development proposals that are located on or near archaeological or paleontological resources to provide a cultural resources study that assesses potential impacts to the resource as a result of the proposed development. The report will include measures to avoid destruction of any significant cultural resources.</i>	See response to Resource Management Goal 7, above
Policy 7.6	<i>Require the preservation of identified cultural resources to the extent possible, prior to development, through dedication, removal, transfer, reuse, or other means.</i>	See response to Resource Management Goal 7, above
Goal 9	<i>Create a network of open space areas and linkages throughout the Planning Area that serves to preserve natural resources, protect health and safety, contributes to the character of the community, provide active and passive recreational use, as well as visual and physical relief from urban development.</i>	This goal is not applicable to the project, as the site is located between the airport runway and the freeway (I-215) and lacks the potential for significant open space linkages.
Policy 9.8	<i>Enforce the standards of the military and FAA relative to aviation hazard areas to protect the use of the aviation field, and use of property within its vicinity.</i>	The project is designed to incorporate appropriate uses as defined in the Air Installation Compatible Use Zone (AICUZ) Study, Airport Layout Plan, Department of Defense Instructions and Air Force Instructions. . The project's land use plan and development regulations include height limitations associated with the 35-foot building restriction line and the 7:1 height requirements. In addition, the proposed uses comply with the Zone B occupancy and use restrictions. Security fencing will be provided adjacent to the runway. In addition, no standing water exceeding the airport's criteria will be allowed as part of the project's drainage plans.

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Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table

General Plan Goal/Policy		Consistency
Resource Management		
Goal 10	<i>Establish standards for scenic corridors, trails and vistas that contribute to the quality of the planning area.</i>	Not applicable. No established scenic corridors or planned trails are present adjacent to the Specific Plan area.
Policy 10.4	<i>Use design standards for transportation facilities that include street trees, pedestrian walkways, bicycle lanes, signing, lighting and setbacks to complement and enhance the character of the planning area.</i>	The Specific Plan includes landscape sections and a landscape plan that includes street trees and pedestrian walkways as well as landscaped setbacks.

General Plan Goal/Policy		Consistency
Safety/Risk Management		
Goal 1	<i>Minimize injury and loss of life, property damage, and other impacts caused by seismic shaking, fault rupture, ground failure, and landslides.</i>	No Fault Rupture Hazard Zone or Alquist-Priolo Earthquake Fault Zone, as designated by the Department of Conservation (DOC), exist within the project site based on the project's soils report and the General Plan. Construction of the project would be required to meet California Building Code (CBC) standards. Additionally, the March JPA would review and approve the plans and specifications of the project to ensure compliance with the provisions of the CBC and Title 24, which regulates building standards, Title 24 is administered by the California Building Standards Commission, which, by law, is responsible for coordinating all building standards.
Policy 1.1	<i>Require geological and geotechnical investigations in areas of potential seismic or geologic hazards as part of the environmental and development review process. Require mitigation of seismic or geologic hazards to the satisfaction of the responsible agencies.</i>	Construction of the project would be required to meet California Building Code (CBC) standards. Additionally, the March JPA would review and approve the plans and specifications of the project to ensure compliance with the provisions of the CBC and Title 24, which regulates building standards, Title 24 is administered by the California Building Standards Commission, which, by law, is responsible for coordinating all building standards.

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Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table

General Plan Goal/Policy		Consistency
Safety/Risk Management		
Policy 1.2	<i>Ensure all grading plans comply with the Uniform Building Code (UBC) and California Building Code including, if necessary, requiring preliminary investigations of development sites by a State-registered geotechnical engineers and certified engineering geologists.</i>	Construction of the project would be required to meet California Building Code (CBC) standards. Additionally, the March JPA would review and approve the plans and specifications of The project to ensure compliance with the provisions of the CBC and Title 24, which regulates building standards, Title 24 is administered by the California Building Standards Commission, which, by law, is responsible for coordinating all building standards.
Goal 2	<i>Minimize grading and otherwise changing the natural topography, while protecting the public safety and property from geologic hazards.</i>	The project will be constructed in a relatively flat area and will not impact the hillsides or rock outcroppings. The project will incorporate grading development standards and recommendations, which will minimize any potential geotechnical and site development constraints that occur on-site.
Policy 2.1	<i>Discourage any grading beyond that which is necessary to create adequate building pads area.</i>	See response to Safety Risk/Management Goal 2.
Goal 3	<i>Minimize injury, loss of life, property damage, and economic and social disruption caused by flood hazards.</i>	See response to Land Use Goal 17.
Policy 3.4	<i>Ensure that development does not divert storm water run off onto adjacent properties, or cause alterations of natural drainage courses that cannot be adequately handled by existing drainage facilities or the flood control improvements proposed with the development.</i>	The project will implement a drainage system that will convey existing storm water around the development footprint and will be consistent with existing drainage patterns. Further, the project will mitigate storm water runoff to a flow rate equivalent to the pre-developed condition. See response to Land Use Goal 17.
Policy 3.5	<i>Require the installation and maintenance of storm drains by property owners.</i>	See response to Land Use Goal 17.

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Veteran’s Industrial Park 215 Specific Plan - General Plan Consistency Table

General Plan Goal/Policy		Consistency
Safety/Risk Management		
Goal 4	<i>Reduce threats to public safety and protect property from wildland and urban fire hazards.</i>	According to the Map My County – Riverside County database (County of Riverside 2015), the project is not within a designated fire hazard area.
Policy 4.5	<i>Ensure that new access roads have adequate widths and turning radius for fire and emergency vehicles.</i>	project roadways meet fire and emergency vehicle standards.
Policy 4.7	<i>Encourage the planting and maintenance of drought-resistant, fire-retardant species on slopes to reduce the risk of brush fire and soil erosion in areas adjacent to hillsides; and develop stringent site design and maintenance standards for areas with high fire hazard.</i>	According to the Map My County – Riverside County database (County of Riverside 2015), the project is not within a designated fire hazard area. In addition, the project will be constructed in a relatively flat area and will not impact hillsides. Furthermore, the project will comply with the Specific Plan Design Guidelines, which require the use of drought-resistant landscaping by the project.
Goal 5	<i>Reduce the potential for hazardous material exposure or contamination in the Planning Area.</i>	The transport of all hazardous materials, is regulated by the U.S. Department of Transportation (Title 49 of the Code of Federal Regulations, the California Highway Patrol (Title 13 of the California Code of Regulations), and the California State Fire Marshall (Title 19 of the California Code of Regulations). In addition, in order to operate in the State of California, all hazardous materials transporters must be registered with the California Department of Toxic Substances Control (DTSC). These regulations minimize the potential for incidents involving hazardous materials.
Policy 5.1	<i>Comply with the enforcement of disclosure laws that require all users, producers, and transporters of hazardous materials and wastes to clearly identify such materials at the site, and to notify the appropriate County, State and/or Federal agencies in the event of a violation.</i>	See response to Safety Risk/Management Goal 5.

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Veteran's Industrial Park 215 Specific Plan - General Plan Consistency Table

General Plan Goal/Policy		Consistency
Safety/Risk Management		
Policy 5.3	<i>Require land uses involved in the production, storage, transportation, handling, or disposal of hazardous materials are located a safe distance from land uses that may be adversely impacted by such activities.</i>	See response to Safety Risk/Management Goal 5.
Policy 5.4	<i>Ensure the storage, use and transportation of any hazardous materials complies with the standards set forth within the errata sheets published for each substance.</i>	See response to Safety Risk/Management Goal 5.
Goal 7	<i>Reduce the possible risk of upset, injury, and loss of life, property damage and other impacts associated with an aviation facility.</i>	The project is designed to incorporate appropriate uses as defined in the Air Installation Compatible Use Zone (AICUZ) Study, Airport Layout Plan, Department of Defense Instructions and Air Force Instructions. The project's land use plan and development regulations include height limitations associated with the 35-foot building restriction line and the 7:1 height requirements. In addition, the proposed uses comply with the Zone B occupancy and use restrictions. Security fencing will be provided adjacent to the runway. In addition, no standing water exceeding the airport's criteria will be allowed as part of the project's drainage plans.
Policy 7.1	<i>Ensure development and use of property within the vicinity of airfield complies with appropriate building standards and codes including height restrictions, restrictions on use, setbacks, population densities, insulation and materials, as contained within an approved Comprehensive Land Use Plan (CLUP) and appropriate AICUZ.</i>	See response to Safety Risk/Management Goal 7.

Veterans Industrial Park 216 Specific Plan
General Plan Amendment
Justification Statement

The D2 property, identified in the applications as the Veterans Industrial Park 215 Specific Plan area currently has a General Plan designation of *Aviation*. The project applicant has entered into an agreement with March JPA to purchase and develop the property for non-aviation uses, including logistics and e-commerce uses. The proposed General Plan Amendment would amend the designation to include a Specific Plan Overlay on the Aviation use and in addition would add general warehousing/logistics to the list of uses contemplated. This would bring the proposed use into compliance with the General Plan.

The D2 parcel is located in close proximity to the Van Buren Boulevard interchange and the Perris Commerce Center Specific Plan area (planned for industrial uses, outside the JPA jurisdiction to the south in the City of Perris), both of which provide access to truck routes and the I-215 freeway. The existing road system would be expanded in conformance with the General Plan Circulation Element, connecting Van Buren Boulevard with Western Way. In addition, the D2 parcel has no direct access to the runways of the airport itself and there are no plans to add this runway access, thus limiting the potential for aviation uses supporting the airport. The proposed uses would fully comply with the noise, population, and height limitations of the airport zones (B2 zone and the 7:1 surface), thus presenting an appropriate land use in this location.

**Veteran's Industrial Park Specific Plan
March JPA Zoning Request and General Plan Consistency Table**

General Plan Goal/Policy		Consistency
Land Use		
Goal 1	<i>Land Use Plan provides for a balanced mix of land uses that contribute to the regional setting, can capitalize on the assets of the Planning Area, while insuring compatibility throughout the Planning Area and with regional plans.</i>	The Project provides for logistics/industrial uses. These uses are compatible with the General Plan and the adjacent Perris Valley Commerce Center Specific Plan. The Project will include a General Plan Amendment adding a Specific Plan Overlay designation to the Site. Consistent with the Reuse Plan and General Plan, the project will continue to improve the balance of population and employment in the project vicinity, providing an opportunity for residents to work locally, rather than commute to surrounding areas throughout the region.
Policy 1.1	<i>Provide for a mix of land uses which implement the Base Master Reuse Plan for March AFB; offer a variety of employment opportunities; and capitalizes, enhances and expands upon existing physical and economic assets of the Planning Area.</i>	See response to Land Use Goal 1.
Policy 1.2	<i>Develop and maintain a system of land use designations and zoning districts which will provide locations for commercial, business park, manufacturing, aviation, public, and open space uses, and which actuates compatible and synergistic land uses.</i>	See response to Land Use Goal 1.

**Veteran’s Industrial Park Specific Plan
March JPA Zoning Request and General Plan Consistency Table**

General Plan Goal/Policy		Consistency
<i>Policy 1.3</i>	<i>Provide for patterns of land use which can be supported by existing and planned circulation, public facilities, and infrastructure system improvements in a manner that will preserve the March JPA’s fiscal capacity.</i>	<p>The Project is consistent with the approved General Plan circulation plan. The Project will extend Van Buren Boulevard south of the March Air Museum. The General Plan designates the Van Buren Boulevard extension as a Major Arterial. This segment of Van Buren Boulevard will be designed as a Modified Secondary Highway, which while providing two traffic lanes, one bicycle lane and sidewalks in each direction, provides 91-feet of right-of-way instead of 100-feet of right-of-way the General Plan assigned to a Major Arterial. An additional 20-foot separation between the edge of sidewalk and screen walls associated with the Veterans Park Specific Plan.</p> <p>In addition, the Project EIR will evaluate the utility providers’ ability to serve the Project. Any significant impacts to public services or utilities will be mitigated through the environmental review process and prior to development.</p>
<i>Policy 1.4</i>	<i>Use specific and/or master plan processes for the coordinated development of large properties to ensure cohesive, comprehensive development.</i>	<p>The property does not currently have a zoning designation. The General Plan land use is Aviation, which would allow for a variety of uses including hangars, aviation support services, air cargo storage, fixed based operations, and aviation operations services. The Site does not have direct access to flying facilities. As such, industrial facilities described in the Specific Plan will allow for a mix of logistics center uses that could support e-commerce, wholesale, storage, distribution, manufacturing and/or assembly centers. It is anticipated that these uses would support airport-related business.</p>
<i>Policy 1.5</i>	<i>Provide for a variety of industrial uses, including heavy manufacturing, light manufacturing, warehousing and distribution, transportation - related, and research and development.</i>	<p>The Project will provide a mix of logistics center uses that could support e-commerce, wholesale, storage, distribution, manufacturing and/or assembly centers.</p>
<i>Policy 1.6</i>	<i>Locate and group commercial and industrial uses which are oriented toward regional service/market areas to promote utilization of regional transportation facilities and development-supporting infrastructure.</i>	<p>See response to Land Use Goal 1.</p>

**Veteran’s Industrial Park Specific Plan
March JPA Zoning Request and General Plan Consistency Table**

General Plan Goal/Policy		Consistency
<i>Policy 1.9</i>	<i>Plan for compatible land uses within the aircraft noise impact contours depicted in the Air Installation Compatible Use Zones (AICUZ) Report for the airfield use.</i>	The proposed concept does not include specific aviation-related facilities, however, the development concept could support airport related businesses. Building height and uses described within the Specific Plan are consistent with the AICUZ guidelines and requirements.
Goal 2	<i>Locate land uses to minimize land use conflict or creating competing land uses, and achieve maximum land use compatibility while improving or maintaining the desired integrity of the Planning Area and subregion.</i>	The Project provides the same types of land uses as designated in the General Plan. These land uses are compatible with the surrounding land uses. Incompatible or competing land uses will not be allowed in the project area.
<i>Policy 2.1</i>	<i>Avoid conflicts and incompatibilities between land uses through the use of landscaped setbacks and buffers, site design, site orientation, architectural features, walls or fences, density/intensity reductions, reduced hours of operation for commercial and industrial uses, shielding of lighting, and the like.</i>	The Design Guidelines provide the architectural, signage, walls and fences, lighting, and landscaping standards to help alleviate any land use incompatibilities with the surrounding areas.
<i>Policy 2.3</i>	<i>Support land uses that provide a balanced land use pattern of the Planning Area, and discourage land uses that conflict of compete with the services and/or plans of adjoining jurisdictions.</i>	See response to Land Use Goal 2.
<i>Policy 2.4</i>	<i>Protect the interests of, and existing commitments to adjacent residents, property owners, and local jurisdictions in planning land uses.</i>	See response to Land Use Goal 2.

**Veteran’s Industrial Park Specific Plan
March JPA Zoning Request and General Plan Consistency Table**

General Plan Goal/Policy		Consistency
Goal 3	<i>Manage growth and development to avoid adverse environmental and fiscal effects.</i>	Development of the Project is bound by the terms and conditions of the Development Agreement between the Master Developer and the March JPA. The Project will be required to implement the applicable infrastructure and services per the Development Agreement. The Development Agreement accommodates a number of financing strategies to fund public improvements and minimize fiscal impacts.
Policy 3.1	<i>Manage growth so that its rate does not exceed the ability of March JPA or service districts to provide for an acceptable level of public facilities and services.</i>	The Project EIR will evaluate the utility providers’ ability to serve the Project. Any significant impacts to public services or utilities will be mitigated through the environmental review process and prior to development.
Goal 4	<i>Develop an identity and foster quality development within the Planning Area.</i>	The Specific Plan Design Guidelines will provide the architectural, signage, parking, and landscaping standards to achieve the goals of both project identity and quality development.
Policy 4.1	<i>Develop and maintain a land use plan for the Planning Area which proposes compatible land uses to create distinct, identifiable historic, commercial, industrial, public, and aviation areas.</i>	See Response to Land Use Goal 1.
Policy 4.4	<i>Develop a distinctive community identity for commercial , business park and industrial developments that reflect the character and atmosphere of March JPA Planning Area through the use of good planning and design principals , and sound development practices which serve as guidelines for building materials, colors, site design and orientation, and landscaping.</i>	See responses to Land Use Goals 1 and 4.

**Veteran’s Industrial Park Specific Plan
March JPA Zoning Request and General Plan Consistency Table**

General Plan Goal/Policy		Consistency
Policy 4.7	<i>Develop and enhance the economic climate and create a balanced business community to serve the work force, commerce and industry of the region.</i>	See response to Land Use Goal 1.
Goal 5	<i>Maximize and enhance the tax base and generation of jobs through new, reuse and joint use opportunities.</i>	The Project will expand on the large employment center concept started by the adjacent Meridian Specific Plan and Perris Valley Commerce Center Specific Plan. As such, it will provide a substantial enhancement to the tax base.
Policy 5.1	<i>Support the development and establishment of new employment centers and economic development activities that contribute to an improved tax base.</i>	See response to Land Use Goal 5.
Policy 5.2	<i>Encourage and facilitate the creation of public/private partnerships that will invest in, and further the implementation of the March AFB Master Reuse Plan.</i>	Riverside Inland Development has entered into a Memorandum of Understanding with March JPA related to development of the property within the Specific Plan area. The applicant, Riverside Inland Development, has assumed the responsibilities and obligations of the Master Developer for the remaining areas of the former March Air Force Base (which includes this Project area) in a Disposition and Development Agreement which will be processed concurrently with the Veterans Industrial Park Specific Plan.
Policy 5.5	<i>Encourage the development of commercial, business park and industrial centers to expand the employment and fiscal base of the March JPA Planning Area and the western Riverside County Subregion.</i>	See response to Land Use Goal 1.
Goal 6	<i>Support the continued Military Mission of March Air Reserve Base, and preservation of the airfield from incompatible land use encroachment.</i>	The Project is designed to incorporate appropriate uses as defined in the Air Installation Compatible Use Zone (AICUZ) Study.

**Veteran's Industrial Park Specific Plan
March JPA Zoning Request and General Plan Consistency Table**

General Plan Goal/Policy		Consistency
Policy 6.3	<i>Ensure that plans and development do not interfere, conflict or degrade the military mission of March ARB.</i>	See response to Land Use Goal 6.
Policy 6.4	<i>Ensure that plans and development do not conflict with the long-term needs of the Air Force Reserve in terms of encroachment, noise, accident zone, constraints, etc.</i>	See response to Land Use Goal 6.
Policy 6.5	<i>Ensure that plans and development conform to the draft Comprehensive Land Use Plan for March AFB/March Inland Port.</i>	See response to Land Use Goal 6.
Policy 6.8	<i>Ensure that land uses adhere to both military and civilian Part 77 conical surface criteria, relative to height restrictions.</i>	The Project will obtain concurrence letters from the FAA stating, “no hazard to air navigation” for the proposed buildings within the development.
Goal 7	<i>Maximize the development potential as a regional Intermodal Transportation facility to support both passenger and freight related air services</i>	
Policy 7.6	<i>Plan for compatible land uses within the aviation area.</i>	The Veterans Industrial Park Specific Plan includes industrial land uses compatible with the adjacent aviation uses through compliance with runway height limitations, provision of security fencing, and provision of warehousing, logistics, and e-commerce uses which could support aviation-related business.
Goal 8	<i>Preserve the natural beauty, minimize degradation of the March JPA Planning Area, and provide enhancement of environmental resources and scenic vistas.</i>	There are no known sensitive environmental areas near the Site. A jurisdictional drainage channel is present, crossing the site; impacts to this drainage will be addressed through a permitting process with appropriate agencies. Design features and mitigation measures to minimize the impacts to potential sensitive land uses from the Project will be evaluated during the environmental review process.

**Veteran's Industrial Park Specific Plan
March JPA Zoning Request and General Plan Consistency Table**

General Plan Goal/Policy		Consistency
<i>Policy 8.1</i>	<i>Where practical, revegetate graded area with native plants compatible to the area to prevent erosion.</i>	The Project will comply with the requirements of the California Construction General Permit (SWRCB Orders No. 2009-009-DWQ as amended by Order 2010-0014-DWQ and Order 2012-006-DWQ) and employ Best Management Practices (BMPs) to minimize erosion for graded areas.
Goal 10	<i>Avoid undue burdening of infrastructure, public facilities, and services by requiring new development to contribute to the improvement and development of the March JPA Planning Area.</i>	See response to Land Use Goal 3.
<i>Policy 10.1</i>	<i>Require new construction to pay its "fair share" of the cost of providing adequate public services, infrastructure, and facilities for the development.</i>	See response to Land Use Goal 3. The project sponsors will finance the public service extensions to water and sewer lines to serve the project, as well as extending Van Buren Boulevard adjacent to the site. In addition, the Project EIR will evaluate traffic and transportation impacts. Any significant impacts to traffic will be mitigated through the environmental review process and prior to development. Further, the project will pay its fair share of the cost of providing services and infrastructure through payment of the development impact fees assessed by the JPA.
<i>Policy 10.2</i>	<i>Require new construction to provide adequate infrastructure to serve the development (i.e., curbs and gutters, sidewalks, street lights, water service, sewer service or septic systems, etc.) prior to initiation of use.</i>	See response to Land Use Goal 3.
<i>Policy 10.3</i>	<i>Locate commercial and industrial development in areas where street rights-of-way and capacity are available, as well as sufficient infrastructure and public services.</i>	See response to Land Use Goal 3.

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General Plan Goal/Policy		Consistency
Goal 12	<i>Ensure, plan, and provide adequate infrastructure for all facility reuse and new development, including but not limited to, integrated infrastructure planning, financing and implementation.</i>	See response to Land Use Goal 3.
Policy 12.3	<i>Require new development projects to provide for the extension of infrastructure to serve the development, including over-sizing facilities for future needs.</i>	See responses to Land Use Policy 1.3 and Goal 3.
Goal 13	<i>Secure adequate water supply system capable of meeting normal and emergency demands for existing and future land uses.</i>	The Project EIR will evaluate the utility providers' ability to serve the Project. Any significant impacts to public services or utilities will be mitigated through the environmental review process and prior to development. A Water Supply Assessment will be prepared as part of project entitlement by Western Municipal Water District to evaluate the required water supply for the subject logistics project.
Policy 13.2	<i>Enhance local groundwater supplies through development designs which promote an on-site recharge and minimize impermeable ground coverage with landscaped areas, open space or recreation areas.</i>	See response to Land Use Goal 17. Note that due to the close proximity to the airport runways the project must minimize standing water while addressing water quality requirements for the site. On-site bio-retention basins will treat on-site runoff while minimizing standing water.
Goal 14	<i>Establish, extend, maintain and finance a safe and efficient wastewater collection, treatment and disposal system, which maximizes treatment and water recharges, minimizes water use, and prevents groundwater contamination.</i>	The Project will provide the necessary conveyance facilities to achieve this goal. The Project EIR will evaluate infrastructure requirements, including conveyance and treatment of wastewater.

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General Plan Goal/Policy		Consistency
Policy 14.1	<i>Require all development to adequately collect, treat, and dispose of wastewater in accordance with the Santa Ana Regional Water Quality Control Board requirements.</i>	The Project will comply with the March JPA's NPDES New Development and Redevelopment Guidelines for Projects Under the March Joint Powers Authority and the Santa Ana Regional Water Quality Control Board Order No. R8-2002-0011.
Policy 14.2	<i>Require connection to the sewer system for any development occurring on land formerly part of March AFB.</i>	See response to Goal 14 and Land Use Policy 14.1. As shown in Figure 3-6 of the Specific Plan, the project will extend sewer lines to service the project, connecting to the existing sewer lines adjacent to the Specific Plan area.
Policy 14.3	<i>Encourage reuse of reclaimed and treated non-potable water for irrigation and maintenance of recreation areas, landscaping and /open space preservation.</i>	Reclaimed water is not available for use within the project.
Goal 16	<i>Adequate supplies of natural gas and electricity from utility purveyors and the availability of communications services shall be provided within the March JPA Planning Area.</i>	The Project EIR will evaluate the utility providers' ability to serve the Project. Any significant impacts to public services or utilities will be mitigated through the environmental review process and prior to development.
Policy 16.1	<i>Where feasible, require new development to underground on-site telecommunication connections.</i>	The Project will underground on-site telecommunication connections.

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General Plan Goal/Policy		Consistency
Goal 17	<i>Adequate flood control facilities shall be provided prior to, and concurrent with, development in order to protect the lives and property within the March JPA Planning Area.</i>	The Project will comply with the March JPA’s NPDES New Development and Redevelopment Guidelines for Projects Under the March Joint Powers Authority and the Santa Ana Regional Water Quality Control Board Order No. R8-2002-0011. A hydrology study and water quality management plan (WQMP) will be prepared for the Project. It is not presumed that Project implementation would have a substantial impact on water quality standards or waste discharge requirements. The project will provide for a drainage plan to convey on-site flows to the existing drainage areas downstream of the project. A regional drainage solution is in process to convey off-site flows around the project site to the existing drainage facilities downstream. Any significant impacts to hydrology and water quality will be mitigated through the environmental review process and prior to development.
<i>Policy 17.1</i>	<i>Provide for the adequate drainage of storm runoff to protect the lives and property within the Planning Area.</i>	See response to Land Use Goal 17.
<i>Policy 17.2</i>	<i>Monitor and maintain drainage and flood control facilities to ensure adequate capacity to support the land use plan.</i>	See response to Land Use Goal 17.
<i>Policy 17.3</i>	<i>Require new development to construct new or upgrade existing drainage facilities to accommodate the additional storm runoff caused by the development.</i>	The project’s hydrology study evaluates the system required to capture and convey on-site runoff. Site runoff will be captured and detained in two on-site basins, one located in each of the two planning areas within the Specific Plan. Water will be detained, treated, and released at a rate consistent with the existing condition.

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Transportation		
Goal 1	<i>Establish and provide for a comprehensive transportation system that captures the assets and opportunities of the planning area, existing transportation facilities, and planned transportation facilities for the future growth and development of the planning area and sub-region.</i>	The Project is located to the east of the Meridian Specific Plan. The existing infrastructure for the Meridian development will be extended to the east to support the Project, including extension of Van Buren Boulevard from its existing terminus to the extension of Western Way on the Specific Plan's southern edge. This road extension is consistent with the planned roadway network in the General Plan Circulation Element.
Policy 1.1	<i>Plan for a mix of transportation modes aimed at effective use of resources, both physical infrastructure and natural energy resources.</i>	See responses to Transportation Goals 1 and 2.
Policy 1.2	<i>Design transportation improvements which are compatible with the natural environment. Xeriscape and drought tolerant landscaping techniques should be used for all parkway and median plantings. Where feasible non-potable water should be used for irrigation purposes.</i>	See responses to Land Use Policy 14.3 and Resource Management Policy 1.5. The Specific Plan's landscape plan illustrates the proposed landscaping within the Specific Plan Area in the proposed roadways supporting the project. The plant palette outlined in Table 4-1 of the Specific Plan is comprised of drought tolerant landscape materials.
Policy 1.4	<i>Roadway system shall inter-relate with the components of the multi-faceted transportation system that will assist with the synergistic value of each element's effectiveness (i.e., bike lockers at the Metrolink station, with bike lanes emanating there from).</i>	See responses to Transportation Goals 1 and 2. No bikeways are planned in the General Plan as part of the extension of Van Buren Boulevard west of I-215, although on-site bicycle parking is provided.
Goal 2	<i>Build and maintain a transportation system which capitalizes on the multi-faceted elements of transportation planning and systems, designed to meet the needs of the planning area, while minimizing negative effects on air quality, the environment and adjacent land uses and jurisdictions.</i>	No bikeways are planned in the General Plan as part of the extension of Van Buren Boulevard west of I-215, although on-site bicycle parking is provided.

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<p><i>Policy 2.6</i></p>	<p><i>FAA Standards, military AICUZ, and appropriate Comprehensive Land Use Plan for March Airfield shall be upheld and supported to encourage and realize a safe environment in and around the aviation field.</i></p>	<p>FAA standards and AICUZ requirements will be complied with. The project's land use plan and development regulations include height limitations associated with the 35-foot building restriction line and the 7:1 height requirements. In addition, the proposed uses comply with the Zone B occupancy and use restrictions. Security fencing will be provided adjacent to the runway. In addition, no standing water exceeding the airport's 48 hour criteria will be allowed as part of the project's drainage plans.</p>
<p><i>Policy 2.7</i></p>	<p><i>On-street parking shall be de-emphasized throughout the planning area to permit maximum capacity of roadways to be actuated by vehicular and bicycle transportation modes.</i></p>	<p>The Project will provide adequate off-street parking to limit the potential for on-street parking. This will allow for more capacity on the roadways for sidewalks and landscaping.</p>
<p><i>Policy 2.8</i></p>	<p><i>Street improvements shall be designed in a comprehensive manner to include parkway facilities, pedestrian walkways, commuter bike lanes, signing, lighting noise and air quality factors, as applicable.</i></p>	<p>The Project will comply with the roadway standards within the Circulation Plan associated with this Specific Plan and its associated Design Guidelines. The Specific Plan specifies the cross-sections for public roadways which include sidewalks, medians, and landscaping. The Design Guidelines provide the architectural, signage, parking, and landscaping standards to achieve the goals of both project identity and quality development. For the Van Buren Boulevard extension to the west of I-215, the General Plan does not include on-street bike lanes.</p>
<p>Goal 3</p>	<p><i>Develop a transportation system that is safe, convenient, efficient and provides adequate capacity to meet local and regional demands.</i></p>	<p>This Project will construct an internal driveway/private drive network and provide for the extension of Van Buren Boulevard from its existing terminus to a planned extension of Western Way on the Specific Plan's southern edge, based on future demand and the General Plan circulation element. Transportation improvements will be constructed and phased as determined by the Project EIR's traffic analysis. The Project EIR will evaluate traffic and transportation impacts. Any significant impacts to traffic will be mitigated through the environmental review process and prior to development.</p>
<p><i>Policy 3.1</i></p>	<p><i>Follow standards for transportation element roadways in designing and constructing street improvements.</i></p>	<p>See response to Transportation Policy 2.8.</p>

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<p>Goal 4</p>	<p><i>Provide a balanced transportation system that ensures the safe and efficient movement of people and goods throughout the planning area, while minimizing the use of land for transportation facilities.</i></p>	<p>Project internal drives and adjacent public roadways (Van Buren Boulevard and Western Way extensions) will be sized to accommodate projected future traffic in an efficient manner.</p>
<p><i>Policy 4.2</i></p>	<p><i>All streets shall be constructed in accordance with planning area's standard street classifications. Modifications within the Northeast Planning Subarea to the standard street classifications may be allowed on a limited basis to preserve the integrity of the area and facilities.</i></p>	<p>See response to Transportation Policy 2.8.</p>
<p><i>Policy 4.10</i></p>	<p><i>Work with the City of Perris to plan for an arterial roadway on the east frontage of I-215 between Van Buren Boulevard and Oleander Avenue, in order to preserve future options for developing a passenger or air cargo terminal on the west side of the runway.</i></p>	<p>The proposed extension of Van Buren Boulevard on the eastern edge of I-215, connecting with a proposed extension of Western Way in the City of Perris, implements this requirement.</p>
<p>Goal 6</p>	<p><i>Establish vehicular access control policies in order to maintain and insure the effectiveness and capacity of arterial roadways.</i></p>	<p>Project internal roadways will be designed in accordance with the "County Road Improvement Standards and Specifications," published by the County of Riverside, and take into account additional landscaping requirements established in the Riverside County Integrated Plan County standards, and implement appropriate intersection and driveways intervals on arterial roadways.</p>
<p><i>Policy 6.2</i></p>	<p><i>Access to an arterial road shall be limited to one point for every 300 feet of frontage or one point for parcels with less than 300 feet of frontage.</i></p>	<p>The Specific Plan area includes approximately 5,500 linear feet of frontage on the proposed extension of Van Buren Boulevard, allowing for approximately 18 access points. The Specific Plan land use includes six points of access and is thus consistent with this requirement.</p>

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Goal 7	<i>Facilitate and develop transportation demand management and transportation systems management programs, and use of alternate transportation modes.</i>	Transportation Demand Management (TDM) strategies will be implemented to shift trips outside the standard commuting hours and/or to non-“drive alone” modes of travel. This is accomplished through various employer-initiated measures, such as flexible working hours, encouragement of carpooling, and facilitating access for non-motorized (i.e., bicycling or walking) modes of travel.
<i>Policy 7.5</i>	<i>Provide a system of bicycle facilities (paths, lanes and routes) in conjunction with circulation system roadway improvements.</i>	No bikeways are planned in the General Plan as part of the extension of Van Buren Boulevard west of I-215, although on-site bicycle parking is provided.
Goal 8	<i>Adequate, affordable, equitably distributed and energy efficient public and mass transit services which promote the mobility to, from, and within the planning area shall be provided.</i>	See response to Transportation Goal 2. The local transit system of bus stops and bus shelters will be approved by the Riverside Transit Agency (RTA) as appropriate along the proposed extension of Van Buren Boulevard and Western Way.
<i>Policy 8.1</i>	<i>Evaluate transportation alternatives with project design, development and implementation.</i>	See responses to Transportation Goals 1 and 2.
Goal 9	<i>Develop measures which will reduce the number of vehicle-miles traveled during peak travel periods.</i>	See response to Land Use Goal 1. The Project will provide a large employment base, which will provide an opportunity for residents in the vicinity to work locally, rather than commute to Los Angeles or Orange Counties. This improved Jobs/housing balance will help reduce vehicle miles traveled.
<i>Policy 9.1</i>	<i>Provide incentives to employers who encourage carpooling and vanpooling for employees.</i>	See response to Transportation Goal 7.
<i>Policy 9.2</i>	<i>Provide preferential parking for carpools and vanpools, where appropriate.</i>	The Specific Plan development regulations require a minimum of 5% carpool/vanpool parking spaces within the Specific Plan area.
Goal 10	<i>Regulate the travel of trucks on March JPA Planning Area streets.</i>	The Project is designed to accommodate truck traffic. Western Way is a designated truck route in the City of Perris, and Van Buren Boulevard is a truck route as designated in the General Plan.

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Policy 10.1	<i>Establish a truck route system which designates truck and commercial vehicle routes and provides adequately sized and designed roadways to meet the needs of trucks and commercial vehicles. This will eliminate truck and commercial vehicle traffic through inappropriate areas of the March JPA Planning Area.</i>	See response to Transportation Goal 10.
Policy 10.2	<i>Clearly sign designated truck routes and identify maximum weight limitations on these routes.</i>	See response to Transportation Goal 10. This requirement would be implemented as part of construction plans for the public roadways developed as part of the project (Van Buren Boulevard and Western Way).
Goal 11	<i>Adequate off-street parking for all land uses shall be provided which requires adequate on-site parking to prevent spill over on the adjacent street system.</i>	The Project will provide adequate parking to limit the potential for parking spillover on to streets. Parking will be provided in accordance with the Development Regulations established by the Specific Plan. Parking ratios have been provided based on square footage. Parking ratios for car/vanpools, bicycle parking, and charging stations have also been provided in the Specific Plan development regulations.
Policy 11.1	<i>Provide for adequate parking facilities for all uses.</i>	See response to Transportation Goal 11.
Policy 11.4	<i>Provide for adequate parking facilities for all uses.</i>	See response to Transportation Goal 11.
Policy 11.5	<i>Provide adequate loading areas within off-street parking areas for all commercial and manufacturing land uses.</i>	The Project will provide adequate loading areas associated with the proposed logistics/industrial buildings.
Goal 12	<i>Plan for and seek to establish and area-wide system of bicycling trails, with linkages within the planning area and with adjacent jurisdictions, and in compliance with sub-regional plans.</i>	No bikeways are planned in the General Plan as part of the extension of Van Buren Boulevard west of I-215, although on-site bicycle parking is provided within the Specific Plan area.

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Policy 12.7	<i>Require sidewalks on both sides of all streets. The March JPA encourages alternate designs including parkways and meandering and enhanced paving.</i>	As shown in the street cross sections of the Specific Plan’s Circulation Plan, sidewalks are provided on the project-frontage of Van Buren Boulevard, and right of way accommodating sidewalks is provided on both Van Buren Boulevard and Western Way extensions.
Goal 13	<i>Promote, preserve, and protect the joint use of the aviation field by the Air Force Reserves and civilian aviation.</i>	The proposed project will not impact the use of the air field in any way.
Policy 13.6	<i>Protect flight paths from inappropriate development encroachment.</i>	FAA standards and AICUZ requirements will be complied with. The project’s land use plan and development regulations include height limitations associated with the 35-foot building restriction line and the 7:1 height requirements. In addition, the proposed uses comply with the Zone B occupancy and use restrictions. Security fencing will be provided adjacent to the runway.
Policy 13.8	<i>Adhere to approved airport layout plans approved by the Joint Powers Commissions and recognized by the FAA.</i>	See response to Policy 13.6 above.
Goal 15	<i>In accordance with state and federal law, promote and provide mobility for the disabled.</i>	Development plans shall take into account the accessibility requirements of the Americans with Disability Act (ADA).
Policy 15.1	<i>Require that all development comply with the requirements of the state and federal law for the disabled. Requirements may include ramps at street corners, access to public buildings, traffic signal timing and the like.</i>	See response to Transportation Goal 15.
Noise		
Goal 1	<i>Ensure that land uses are protected from excessive and unwanted noise.</i>	Project development shall be consistent with the land use limitations established in the AICUZ study and the Riverside County Airport Land Use Plan. Industrial uses are consistent with the noise limitations of the AICUZ study.

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<i>Policy 1.1</i>	<i>Establish acceptable limits of noise for various land uses throughout the March JPA Planning Area. Future development that could increase ambient noise levels shall be required to mitigate the anticipated noise increase, to the extent possible.</i>	The Project EIR will evaluate noise impacts. The Specific Plan area is located adjacent to the airport runway, an inherently noisy environment. Any significant impacts from Project noise will be mitigated through the environmental review process and prior to development. Industrial uses are consistent with the noise limitations of the AICUZ study.
<i>Policy 1.3</i>	<i>Encourage good acoustical design in new construction.</i>	See response to Noise Policy 1.1.
Goal 2	<i>Minimize incompatible noise level exposures throughout the Planning Area, and where possible, mitigate the effect of noise incompatibilities to provide a safe and healthy environment.</i>	No sensitive uses are located in proximity to the proposed Specific Plan area. Design features and mitigation measures to minimize noise impacts from the Project will be evaluated during the environmental review process. Incompatible land uses will not be allowed in the project area.
<i>Policy 2.4</i>	<i>March JPA shall evaluate noise sensitivity and noise generation when considering land use projects and transportation improvement projects, and where appropriate mitigation measures shall be employed.</i>	See response to Noise Goal 2. Industrial uses are consistent with the noise limitations of the AICUZ study.
Goal 3	<i>Work toward the reduction of noise impacts from vehicular traffic, and aviation and rail operations.</i>	See response to Noise Goal 2.
<i>Policy 3.4</i>	<i>Where appropriate, noise mitigation measures shall be incorporated in the design and approval of development property located adjacent to aviation and rail facilities.</i>	Noise mitigation, if required, will be incorporated into the project based on the noise studies prepared as part of the project’s Environmental Impact Report.
<i>Policy 3.7</i>	<i>Limit trucking operations to appropriate routes, times and speeds.</i>	The Project is designed to accommodate truck traffic. Western Way is a designated truck route in the City of Perris, and Van Buren Boulevard is a truck route as designated in the General Plan. Standard speeds would apply on public truck routes.

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Policy 3.8	<i>Appropriate muffling systems for construction equipment and operations shall be required, as necessary.</i>	The Project EIR will evaluate noise impacts associated with construction. Any significant impacts from Project construction noise will be mitigated through the environmental review process and prior to development.
Air Quality		
Goal 1	<i>Promote alternative modes of travel.</i>	See response to Transportation Goal 2.
Policy 1.3	<i>Support trip-reduction programs, such as longer work days, shorter week work schedules.</i>	See response to Transportation Goal 7.
Goal 2	<i>Reduce emissions associated with vehicle miles traveled by enhancing the jobs/housing balance of the subregion of western Riverside County.</i>	The Project will provide a large employment base, which will provide an opportunity for residents in the vicinity to work locally, rather than commute to Los Angeles or Orange Counties. This improved Jobs/housing balance will help reduce vehicle miles traveled.
Policy 2.1	<i>Create an employment center within the housing rich environment of the subregion.</i>	See responses to Air Quality Goal 1 and Transportation Goal 9.
Goal 3	<i>Reduce air pollution through proper land use, transportation and energy use planning.</i>	See responses to Transportation Goals 2 and 7.
Policy 3.1	<i>Locate ancillary uses within business and employment centers to reduce the number of vehicle trips and lessen the vehicle miles traveled.</i>	The Specific Plan development regulations allow for ancillary office and retail uses in association with the proposed logistics/e-commerce uses. Inclusion of these ancillary uses will reduce the need for outside vehicular trips.
Policy 3.2	<i>Locate service uses and facilities in convenient proximity to employment and business center areas to encourage pedestrian or alternative transit to reduce the number of vehicle trips.</i>	See responses to Land Use Goal 1 and Transportation Goal 9.

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<i>Policy 3.3</i>	<i>Develop a bike lane network that will link the bike lanes to residential areas adjacent to the Planning Area and Metrolink Stations to encourage non-motorized travel within the planning area.</i>	No bikeways are planned in the General Plan as part of the extension of Van Buren Boulevard west of I-215, although on-site bicycle parking is provided within the Specific Plan area.
<i>Policy 3.4</i>	<i>Encourage ride share programs.</i>	See response to Transportation Goal 7.
<i>Policy 3.5</i>	<i>Parking facilities shall be designed to safely accommodate and support alternative modes of transportation and preferential location of alternative fuel vehicles and mass transit services.</i>	See response to Transportation Goal 2. In addition, the project will include charging stations for electric vehicles within the parking areas of the plan consistent with CAL Green standards.
Goal 5	<i>Maximize the effectiveness of air quality control programs through coordination with other governmental entities.</i>	The Project will comply with South Coast Air Quality Management District rules and regulations.
<i>Policy 5.5</i>	<i>Review development projects to determine the potential air quality impacts and provide appropriate mitigation, where necessary.</i>	The Project EIR will evaluate air quality impacts. Any significant impacts to air quality from the Project will be mitigated through the environmental review process and prior to development.
Goal 6	<i>Reduce emissions associated with vehicle/engine use.</i>	See responses to Land Use Goal 1 and Transportation Goal 9.
<i>Policy 6.3</i>	<i>Encourage diversion of peak hour truck traffic, whenever feasible, to off-peak periods to reduce roadway congestion and associated emissions.</i>	See response to Transportation Goal 10.
<i>Policy 6.5</i>	<i>Encourage trucks operating within March JPA Planning Area to maintain safety equipment and operate at safe speeds so as to reduce the potential for accidents which create congestion and related emissions.</i>	The Project EIR will evaluate air quality impacts. Any significant impacts to air quality from the Project will be mitigated through the environmental review process and prior to development.

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Goal 7	<i>Reduce emissions associated with energy consumption.</i>	Buildings shall be designed to reduce energy usage through various measures (such as energy efficient lighting and glazing, using lighter colored roofing materials, orienting buildings north and increasing wall insulation above Title 24 requirements, etc.) The project's EIR will analyze the impacts associated with energy consumption and proposed mitigation measures to reduce impacts.
<i>Policy 7.1</i>	<i>Support the use of energy-efficient equipment and design in the March JPA Planning Area for facilities and infrastructure.</i>	See response to Air Quality Goal 7.
<i>Policy 7.3</i>	<i>Support passive solar design in new construction.</i>	See response to Air Quality Goal 7.
<i>Policy 7.4</i>	<i>Support recycling programs which reduce emissions associated with manufacturing and waste disposal.</i>	Recycling of materials will be a component of the waste management program of uses within the Specific Plan area.
<i>Policy 7.5</i>	<i>Support drought-resistant vegetation in landscaping areas to reduce energy needed to pump water.</i>	The specific Plan includes a landscape plan and plant palette for development within the Specific Plan area. The project's plant materials include predominantly drought tolerant vegetation.
Goal 8	<i>Reduce air pollution emissions and impacts through siting and building design.</i>	See response to Air Quality Goal 7. Design features and mitigation measures to minimize the impacts to air quality from the Project will be evaluated during the environmental review process.
<i>Policy 8.1</i>	<i>Support the use of low polluting construction materials and coatings.</i>	The Project EIR will evaluate air quality impacts from construction. Any significant impacts to air quality from the Project will be mitigated through the environmental review process and prior to development.
Goal 9	<i>Reduce fugitive dust and particulate matter emissions.</i>	Trucks hauling dirt, sand, gravel or soil are to be covered or should maintain at least two feet of freeboard in accordance with Section 23114 of the California Vehicle Code. Where feasible, construction access roads to the main roads should be paved to avoid dirt being carried on to the roadway or track-out devices should be installed. In addition, the project EIR will evaluate impacts associated with fugitive dust and particulate matter during construction and proposed mitigation measures to reduce or eliminate impacts.

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<i>Policy 9.1</i>	<i>Require all feasible fugitive dust reduction techniques to be utilized during construction activities.</i>	See response to Air Quality Goal 9.
Housing		
The General Plan does not allow for housing opportunities within the March JPA Planning Area due to incompatible uses with the airfield, the need to focus on the reestablishment of the numerous jobs lost due to base realignment, and the housing rich environment of Western Riverside County. The Project maintains consistency with the General Plan’s absence of a residential land use designation within the Specific Plan area.		
Resource Management		
Goal 1	<i>Conserve and protect surface water, groundwater, and imported water resources.</i>	The Project will be constructed to mitigate impacts to the existing drainage channel. Furthermore, the Project EIR will evaluate impacts to hydrology and water supply from the Project. Any significant impacts to hydrology or water supply from the Project will be mitigated through the environmental review process and prior to development.
<i>Policy 1.1</i>	<i>Where possible, retain local drainage courses, channels and creeks in their natural condition.</i>	See response to Resource Management Goal 1. The on-site drainage is related to regional water flows across the site and is not a natural stream course. The proposed grading plan impacts this drainage to facilitate development of the site and to provide an alternative drainage conveyance plan for regional flows. Impacts will be mitigated in accordance with agency permitting requirements.
<i>Policy 1.2</i>	<i>Protect groundwater and surface water resources from depletion and sources of pollution.</i>	See responses to Land Use Goal 17 and Resource Management Goal 1.
<i>Policy 1.4</i>	<i>Require development to conserve water resources, including the use of water-efficient plumbing fixtures and irrigation systems.</i>	See response to Air Quality Goal 7.

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<i>Policy 1.5</i>	<i>Conserve imported water by requiring water conservation techniques, water-conserving and recycling processes, drought-resistant landscaping, and reclaimed water for irrigation, when available and appropriate.</i>	The Project will comply with the Specific Plan’s Design Guidelines. The Design Guidelines require the use of drought-resistant landscaping by the Project. Reclaimed water will be used if available.
<i>Policy 1.6</i>	<i>Promote the use of drought tolerant landscaping in development, and encourage the use of reclaimed water for irrigation in parks, golf courses, and industrial uses, as well as for other urban uses, whenever feasible and where legally permitted.</i>	See response to Resource Management Policy 1.5. The Specific Plan includes a landscape plan and plant palette for development within the Specific Plan area. The project’s plant materials include predominantly drought tolerant vegetation. The project does not contain parks.
<i>Policy 1.8</i>	<i>Assure that development projects comply with regulatory agency requirements, including federal, state and regional regulations.</i>	The Project shall be required to obtain a Clean Water Act (CWA) Section 404 permit (regulated by the U.S Army Corps of Engineers), a Streambed Alteration Agreement (regulated by the California Department of Fish and Wildlife), a CWA Section 401 Water Quality Certification (regulated by the Regional Water Quality Control Board) to impact waters of the U.S/State.
Goal 2	<i>Control flooding to reduce major losses of life and property.</i>	See response to Land Use Goal 17.
<i>Policy 2.3</i>	<i>Ensure that development does not divert storm water run off onto adjacent properties, or cause alterations of natural drainage courses that cannot be adequately handled by flood control improvements installed coincident with the development.</i>	See response to Land Use Goal 17.
<i>Policy 2.5</i>	<i>To the greatest extent possible, require development to use master flood control facilities and limit use of interim drainage facilities or open channels.</i>	The drainage plan for the project will utilize the existing open channel as a downstream conveyance system as well as an open trapezoidal channel along the project’s easterly property line as an interim solution to a regional issue. See response to Land Use Goal 17.

**Veteran’s Industrial Park Specific Plan
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Goal 3	<i>Conserve and protect significant land forms, important watershed areas, mineral resources and soil conditions.</i>	The Project will not impact any significant landforms or mineral resources. The Project EIR will evaluate impacts to hydrology, geology and soils from the Project. Any significant impacts to hydrology, geology and soils from the Project will be mitigated through the environmental review process and prior to development.
Policy 3.1	<i>Conserve hillsides and rock outcroppings in the planning area through the use of master-planned developments which create a “Campus-like” setting, and encourage the creative siting of building areas as a means of retaining natural areas and open space.</i>	The Project will be constructed in a flat area with no rock outcroppings and will not impact any of the area’s hillsides.
Policy 3.5	<i>Require and practice proper soil management techniques to reduce erosion, sedimentation and other soil-related problems.</i>	See response to Land Use Policy 8.1. A SWPPP and Erosion Control Plan will be required for the project to reduce erosion and other soil related problems. In addition, all grading will be reviewed by a soils consultant as part of grading permit preparation.
Policy 3.6	<i>Control erosion during and following construction through proper grading techniques, vegetation replanting, and the installation of proper drainage control improvements.</i>	See response to Land Use Policy 8.1 and Resource Management Policy 3.5
Policy 3.7	<i>Require erosion control measures such as binders, revegetation, slope covers, and other practices which reduce soil erosion due to wind and water.</i>	See response to Land Use Policy 8.1 and Resource Management Policy 3.5
Goal 4	<i>Conserve energy resources through use of available energy technology and conservation practices.</i>	See response to Air Quality Goal 7.
Policy 4.1	<i>Implement energy performance requirements established under the California Administration Code Title 24 Energy Conservation and Insulation Regulations.</i>	See response to Air Quality Goal 7.

**Veteran’s Industrial Park Specific Plan
March JPA Zoning Request and General Plan Consistency Table**

<i>Policy 4.2</i>	<i>Encourage innovative building, site design and orientation techniques which minimize energy use by taking advantage of sun/shade patterns, prevailing winds, landscaping, and building materials available to control energy usage.</i>	See response to Air Quality Goal 7.
<i>Policy 4.3</i>	<i>Encourage the use and development of alternative and innovative energy resources and energy conservation techniques, where practical.</i>	See response to Air Quality Goal 7.
Goal 5	<i>Conserve and protect significant stands of mature trees, native vegetation, and habitat within the planning area.</i>	The Project EIR will evaluate impacts to biological resources from the Project. Any significant impacts to biological resources from the Project will be mitigated through the environmental review process and prior to development. The Project would comply with the federal, state and local regulations regarding impacts to sensitive biological resources.
<i>Policy 5.1</i>	<i>Where practical, conserve important plant communities and habitats such as riparian areas, wetlands, significant tree stands, and species by using buffers, creative site planning, revegetation and open space easements/dedications.</i>	See responses to Resource Management Goals 1 and 5.
<i>Policy 5.2</i>	<i>Encourage the planting of native species of trees and other drought-tolerant vegetation.</i>	See response to Resource Management Policy 1.5.
<i>Policy 5.4</i>	<i>In areas that may contain important plant and animal communities, require development to prepare biological assessments identifying species types and locations and develop measures to preserve recognized sensitive species, as appropriate.</i>	See responses to Resource Management Policy 1.8 and Goal 5. The site is not located in an area with important plant and animal communities.

**Veteran's Industrial Park Specific Plan
March JPA Zoning Request and General Plan Consistency Table**

<i>Policy 5.6</i>	<i>Work with state, federal and local agencies in the preservation and/or mitigation of recognized sensitive vegetation and wildlife in March JPA Planning Area.</i>	See response to Resource Management Policy 1.8.
Goal 6	<i>Provide an effective and efficient waste management system for solid and hazardous wastes that is financially and environmentally responsible.</i>	The Project shall comply with appropriate and applicable regulations and standards with respect to the management of solid and hazardous wastes.
<i>Policy 6.4</i>	<i>Coordinate with regulatory agencies in assuring that future development handles and disposes of hazardous materials in compliance with applicable regulations.</i>	See response to Safety Risk/Management Goal 4.
Goal 7	<i>Promote cultural awareness through preservation of the planning area's historic, archaeological and paleontological resources.</i>	The Project is not anticipated to impact significant historic, archaeological or paleontological resources. The project EIR will comply with the requirements of AB 52 related to Tribal consultation.
<i>Policy 7.5</i>	<i>Require development proposals that are located on or near archaeological or paleontological resources to provide a cultural resources study that assesses potential impacts to the resource as a result of the proposed development. The report will include measures to avoid destruction of any significant cultural resources.</i>	See response to Resource Management Goal 7, above
<i>Policy 7.6</i>	<i>Require the preservation of identified cultural resources to the extent possible, prior to development, through dedication, removal, transfer, reuse, or other means.</i>	See response to Resource Management Goal 7, above

**Veteran’s Industrial Park Specific Plan
March JPA Zoning Request and General Plan Consistency Table**

Goal 9	<i>Create a network of open space areas and linkages throughout the Planning Area that serves to preserve natural resources, protect health and safety, contributes to the character of the community, provide active and passive recreational use, as well as visual and physical relief from urban development.</i>	This goal is not applicable to the project, as the site is located between the airport runway and the freeway (I-215) and lacks the potential for significant open space linkages.
<i>Policy 9.8</i>	<i>Enforce the standards of the military and FAA relative to aviation hazard areas to protect the use of the aviation field, and use of property within its vicinity.</i>	FAA standards and AICUZ requirements will be complied with. The project’s land use plan and development regulations include height limitations associated with the 35-foot building restriction line and the 7:1 height requirements. In addition, the proposed uses comply with the Zone B occupancy and use restrictions. Security fencing will be provided adjacent to the runway. In addition, no standing water exceeding the airport’s 48 hour criteria will be allowed as part of the project’s drainage plans.
Goal 10	<i>Establish standards for scenic corridors, trails and vistas that contribute to the quality of the planning area.</i>	Not applicable. No established scenic corridors or planned trails are present adjacent to the Specific Plan area.
<i>Policy 10.4</i>	<i>Use design standards for transportation facilities that include street trees, pedestrian walkways, bicycle lanes, signing, lighting and setbacks to complement and enhance the character of the planning area.</i>	The Specific Plan includes landscape sections and a landscape plan that includes street trees and pedestrian walkways as well as landscaped setbacks.
Safety/Risk Management		
Goal 1	<i>Minimize injury and loss of life, property damage, and other impacts caused by seismic shaking, fault rupture, ground failure, and landslides.</i>	No Fault Rupture Hazard Zone or Alquist-Priolo Earthquake Fault Zone, as designated by the Department of Conservation (DOC), exist within the Project site based on the projects soils report and the General Plan. Construction of the Project would be required to meet California Building Code (CBC) standards. Additionally, the March JPA would review and approve the plans and specifications of the proposed Project to ensure compliance with the provisions of the CBC and Title 24, which regulates building standards, Title 24 is administered by the California Building Standards Commission, which, by law, is responsible for coordinating all building standards.

**Veteran’s Industrial Park Specific Plan
March JPA Zoning Request and General Plan Consistency Table**

Policy 1.1	<i>Require geological and geotechnical investigations in areas of potential seismic or geologic hazards as part of the environmental and development review process. Require mitigation of seismic or geologic hazards to the satisfaction of the responsible agencies.</i>	Construction of the Project would be required to meet California Building Code (CBC) standards. Additionally, the March JPA would review and approve the plans and specifications of the proposed Project to ensure compliance with the provisions of the CBC and Title 24, which regulates building standards, Title 24 is administered by the California Building Standards Commission, which, by law, is responsible for coordinating all building standards.
Policy 1.2	<i>Ensure all grading plans comply with the Uniform Building Code (UBC) and California Building Code including, if necessary, requiring preliminary investigations of development sites by a State-registered geotechnical engineers and certified engineering geologists.</i>	Construction of the Project would be required to meet California Building Code (CBC) standards. Additionally, the March JPA would review and approve the plans and specifications of the proposed Project to ensure compliance with the provisions of the CBC and Title 24, which regulates building standards, Title 24 is administered by the California Building Standards Commission, which, by law, is responsible for coordinating all building standards.
Goal 2	<i>Minimize grading and otherwise changing the natural topography, while protecting the public safety and property from geologic hazards.</i>	The Project will be constructed in a relatively flat area and will not impact the hillsides or rock outcroppings. The Project will incorporate grading development standards and recommendations, which will minimize any potential geotechnical and site development constraints that occur on-site.
Policy 2.1	<i>Discourage any grading beyond that which is necessary to create adequate building pads area.</i>	See response to Safety Risk/Management Goal 2.
Goal 3	<i>Minimize injury, loss of life, property damage, and economic and social disruption caused by flood hazards.</i>	See response to Land Use Goal 17.
Policy 3.4	<i>Ensure that development does not divert storm water run off onto adjacent properties, or cause alterations of natural drainage courses that cannot be adequately handled by existing drainage facilities or the flood control improvements proposed with the development.</i>	The proposed project will implement a drainage system that will convey existing storm water around the development footprint and will be consistent with existing drainage patterns. Further, the project will mitigate storm water runoff to a flow rate equivalent to the pre-developed condition. See response to Land Use Goal 17.

**Veteran’s Industrial Park Specific Plan
March JPA Zoning Request and General Plan Consistency Table**

<i>Policy 3.5</i>	<i>Require the installation and maintenance of storm drains by property owners.</i>	See response to Land Use Goal 17.
Goal 4	<i>Reduce threats to public safety and protect property from wildland and urban fire hazards.</i>	According to the Map My County – Riverside County database (County of Riverside 2015), the Project is not within a designated fire hazard area.
<i>Policy 4.5</i>	<i>Ensure that new access roads have adequate widths and turning radius for fire and emergency vehicles.</i>	Project roadways meet fire and emergency vehicle standards.
<i>Policy 4.7</i>	<i>Encourage the planting and maintenance of drought-resistant, fire-retardant species on slopes to reduce the risk of brush fire and soil erosion in areas adjacent to hillsides; and develop stringent site design and maintenance standards for areas with high fire hazard.</i>	According to the Map My County – Riverside County database (County of Riverside 2015), the Project is not within a designated fire hazard area. In addition, the Project will be constructed in a relatively flat area and will not impact hillsides. Furthermore, the Project will comply with the Specific Plan Design Guidelines, which require the use of drought-resistant landscaping by the Project.
Goal 5	<i>Reduce the potential for hazardous material exposure or contamination in the Planning Area.</i>	The transport of all hazardous materials, is regulated by the U.S. Department of Transportation (Title 49 of the Code of Federal Regulations, the California Highway Patrol (Title 13 of the California Code of Regulations), and the California State Fire Marshall (Title 19 of the California Code of Regulations). In addition, in order to operate in the State of California, all hazardous materials transporters must be registered with the California Department of Toxic Substances Control (DTSC). These regulations minimize the potential for incidents involving hazardous materials.
<i>Policy 5.1</i>	<i>Comply with the enforcement of disclosure laws that require all users, producers, and transporters of hazardous materials and wastes to clearly identify such materials at the site, and to notify the appropriate County, State and/or Federal agencies in the event of a violation.</i>	See response to Safety Risk/Management Goal 5.

**Veteran's Industrial Park Specific Plan
March JPA Zoning Request and General Plan Consistency Table**

Policy 5.3	<i>Require land uses involved in the production, storage, transportation, handling, or disposal of hazardous materials are located a safe distance from land uses that may be adversely impacted by such activities.</i>	See response to Safety Risk/Management Goal 5.
Policy 5.4	<i>Ensure the storage, use and transportation of any hazardous materials complies with the standards set forth within the errata sheets published for each substance.</i>	See response to Safety Risk/Management Goal 5.
Goal 7	<i>Reduce the possible risk of upset, injury, and loss of life, property damage and other impacts associated with an aviation facility.</i>	FAA standards and AICUZ requirements will be complied with. The project's land use plan and development regulations include height limitations associated with the 35-foot building restriction line and the 7:1 height requirements. In addition, the proposed uses comply with the Zone B occupancy and use restrictions. Security fencing will be provided adjacent to the runway. In addition, no standing water exceeding the airport's 48 hour criteria will be allowed as part of the project's drainage plans.
Policy 7.1	<i>Ensure development and use of property within the vicinity of airfield complies with appropriate building standards and codes including height restrictions, restrictions on use, setbacks, population densities, insulation and materials, as contained within an approved Comprehensive Land Use Plan (CLUP) and appropriate AICUZ.</i>	See response to Safety Risk/Management Goal 7.

NOTICE OF PUBLIC HEARING RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Thursday from 8:00 a.m. to 4:30 p.m., except Monday, October 8 (Columbus Day), and by prescheduled appointment on Fridays, from 9:00 a.m. to 5:00 p.m.

ATTENTION: ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. The March Joint Powers Authority will hold hearings on this item and should be contacted on non-ALUC issues.

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside California

DATE OF HEARING: October 11, 2018

TIME OF HEARING: 9:30 A.M.

CASE DESCRIPTION:

ZAP1274MA17 – Riverside Inland Development, LLC/Hillwood Investment Properties (Representative: Kathy Hoffer) – March Joint Powers Authority Case Nos. General Plan Amendment No. 16-01, Specific Plan 16-02, Plot Plan 16-06, Parcel Map 16-02 (Tentative Parcel Map No. 37220). The applicant proposes to construct two industrial buildings totaling 2,185,618 square feet on 142.5 acres located easterly of Interstate 215, southerly of March Field Air Museum and the easterly terminus of Van Buren Boulevard, northerly of Nandina Avenue, and westerly of March Air Reserve Base. The applicant also proposes to amend the March Joint Powers Authority General Plan to include general warehousing/logistics uses as allowable land uses on lands designated as "Aviation" (AV), to amend the site's designation from "AV" to "AV (SP-8)", and to update the Building Capacity table in the Land Use Element. Specific Plan No. 16-02 is a proposal for a new Specific Plan (SP-8) that provides goals, policies, programs, land uses, development standards, and design guidelines for development on this site. Tentative Parcel Map No. 37220 proposes to divide the site into two parcels (one for each building). (Airport Compatibility Zone B2 of the March Air Reserve Base/Inland Port Airport Influence Area).

FURTHER INFORMATION: Contact ALUC Planner Paul Rull at (951) 955-6893. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Mr. Jeffrey Smith of the March Joint Powers Authority at (951) 656-7000.

APPLICATION FOR MAJOR LAND USE ACTION REVIEW
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

ALUC Identification No.

ZAP 1274MAN

March
B2

PROJECT

PROJECT PROPONENT (TO BE COMPLETED BY APPLICANT)

Date of Application June 23, 2017
 Property Owner March Joint Powers Authority Phone Number 951-651-7000
 Mailing Address 14205 Meridian Parkway #0140 KATH
Riverside, CA 92518

Agent (if any) Riverside Inland Development, LLC Phone Number (909) 382-0033
 Mailing Address 901 Via. Piemonte, Suite 175 KATHY HOFFER
Ontario, CA 91764

PROJECT LOCATION (TO BE COMPLETED BY APPLICANT)

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address Van Buren @ I215 @ March Inland Port Airport
 Assessor's Parcel No. 294-150-009, 294-170-005, Parcel Size 142.5 Acres
 Subdivision Name 295-300-008, 294-140-013,
 Lot Number 294-180-038 Zoning Classification Un-zoned

PROJECT DESCRIPTION (TO BE COMPLETED BY APPLICANT)

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe) The existing General Plan land use designation is aviation. The site is located within Zone B2.

Proposed Land Use (describe) The existing General Plan land use designation of aviation would be expanded to include general warehousing and logistics uses.

For Residential Uses Number of Parcels or Units on Site (exclude secondary units) N/A
 For Other Land Uses Hours of Use 24/7 Operation
 (See Appendix C) Number of People on Site Maximum Number 100 people per acre, average over site.
 Method of Calculation Per Specific Plan

Height Data Height above Ground or Tallest Object (including antennas and trees) 48'-0" ft.
 Highest Elevation (above sea level) of Any Object or Terrain on Site 1571 above sea level

Flight Hazards Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight? Yes
 No
 If yes, describe _____

REFERRING AGENCY (APPLICANT OR JURISDICTION TO COMPLETE)	
Date Received	6-26-17
Agency Name	MARCH JPA
Staff Contact	JEFFREY SMITH
Phone Number	951-656-7000
Agency's Project No.	
Type of Project	<input checked="" type="checkbox"/> General Plan Amendment GRA 16-01 <input checked="" type="checkbox"/> Zoning Amendment or Variance SP 16-02 <input checked="" type="checkbox"/> Subdivision Approval TPM 16-06 <input type="checkbox"/> Use Permit <input type="checkbox"/> Public Facility <input checked="" type="checkbox"/> Other Plot Plan PP 16-06

A. **NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. **SUBMISSION PACKAGE:**

ALUC REVIEW

- 1 Completed Application Form
- 1 Project Site Plan – Folded (8-1/2 x 14 max.)
- 1 Elevations of Buildings - Folded
- 1 Each . 8 ½ x 11 reduced copy of the above
- 1 8 ½ x 11 reduced copy showing project in relationship to airport.
- 1 Set . Floor plans for non-residential projects
- 4 Sets . Gummed address labels of the Owner and representative (*See Proponent*).
- 1 Set . Gummed address labels of all property owners within a 300' radius of the project site. If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10), with ALUC return address.
- 4 Sets . Gummed address labels of the referring agency (City or County).
- 1 Check for Fee (See Item "C" below)

STAFF REVIEW (Consult with ALUC staff planner as to whether project qualifies)

- 1 Completed Application Form
- 1 Project Site Plans – Folded (8-1/2 x 14 max.)
- 1 Elevations of Buildings - Folded
- 1 8 ½ x 11 Vicinity Map
- 1 Set . Gummed address labels of the Owner and representative (*See Proponent*).
- 1 Set . Gummed address labels of the referring agency.
- 1 Check for review—See Below

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

STAFF REPORT

ADMINISTRATIVE ITEMS

4.1 Director's Approvals.

- A. During the period of August 18, 2018 through September 13, 2018, as authorized pursuant to Section 1.5.2(d) of the 2004 Riverside County Airport Land Use Compatibility Plan, ALUC Director Simon Housman reviewed one non-legislative case set (2 Planning cases) not associated with legislative cases within Compatibility Zone D and issued a determination of consistency.

ZAP1043TH18 (Jacqueline Cochran Regional Airport, Zone D) pertains to County of Riverside Case Nos. CUP180019 (Conditional Use Permit), a proposal to establish a gas station facility with 12 fueling spaces and a 5,275 square foot convenience store on a 9.67-acre parcel (Assessor's Parcel Number 763-250-040) located on the northeast corner of Airport Boulevard and Harrison Street, and PM37562 (Tentative Parcel Map), a proposal to divide the site into three parcels (with the proposed gas station and convenience store located on a 4.24-acre parcel). Airport Compatibility Zone D limits nonresidential intensity to an average of 100 persons per acre and a maximum of 300 persons in any given single-acre area. The proposed development would accommodate a total of 106 persons (88 persons in the convenience store, based on one person per 60 square feet, and 18 persons at the gas pumps, based on 1.5 persons per vehicle). This results in an average intensity of 25 people per acre on the 4.24-acre proposed parcel, which would be consistent with the Compatibility Zone D criterion of 100 persons per acre as the highest allowable average intensity. The most intense single-acre area would include the convenience store and gas station and accommodate 106 persons, which would be consistent with the Zone D single-acre intensity criterion of 300 people.

The elevation at the northwesterly end of Runway 12-30 at Jacqueline Cochran Regional Airport is 117 feet below mean sea level (MSL). At a distance of 5,838 feet from the runway to the project site, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with a top point elevation exceeding -58.62 feet MSL. The project site elevation is -96 feet MSL and the structure height is 26 feet, resulting in a maximum top point elevation of -70 feet MSL. Therefore, FAA OES review for height/elevation reasons was not required. ALUC Director Simon Housman issued a determination of consistency for this project on September 13, 2018.

A copy of the consistency letter and background documents are attached, for the Commission's information.

4.2 ALUC Director's Report: The Path Forward Following the Release of the 2018 Air Installation Compatible Use Zones Report (2018 AICUZ) for March Air Reserve Base/Inland Port Airport

ALUC Director Simon Housman will continue his briefing of the Commission with an update of the anticipated timeline for the preparation of an Airport Land Use Compatibility Plan in compliance with the 2018 AICUZ.

4.3 Nighttime Aircraft Operations at the March Inland Port Airport: The March Joint Powers Authority has issued a Notice of Availability of [a] Proposed Threshold of Significance Concerning Nighttime Aircraft

Operations at the March Inland Port Airport. Specifically, proposed projects that would result in aircraft operations (landings and/or takeoffs) at the March Inland Port Airport between 10:00 P.M. and 6:59 A.M. that could expose people in the vicinity to a significant risk of sleep disturbance due to noise would be considered to have a significant noise impact. This would affect the type of environmental document that would be required prior to such projects being approved. ALUC Director Simon Housman will address the Commission with regard to a possible ALUC comment on the proposed threshold, focusing on whether the reference to aircraft operations should be limited to operations of specific types of aircraft.

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AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

H.1

September 13, 2018

Mr. Jay Olivas, Project Planner
County of Riverside Planning Department
77588 El Duna Court H
Palm Desert CA 92211

CHAIR
Steve Manos
Lake Elsinore

VICE CHAIR
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Desert Hot Springs

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Riverside

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STAFF

Director
Simon A. Housman

John Guerin
Paul Rull
Barbara Santos

County Administrative Center
4080 Lemon St., 14th Floor
Riverside, CA 92501
(951) 955-5132

www.realuc.org

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –
DIRECTOR’S DETERMINATION**

File No.: ZAP1043TH18
Related File No.: CUP180019 (Conditional Use Permit), PM37562 (Tentative Parcel Map)
APN: 763-250-040

Dear Mr. Olivas:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed County of Riverside Case Nos. CUP180019 (Conditional Use Permit), a proposal to establish a gas station facility with 12 fueling spaces and a 5,275 square foot convenience store on 9.67 acres (Assessor’s Parcel Number 763-250-040) located on the northeast corner of Airport Boulevard and Harrison Street, and PM37562 (Tentative Parcel Map), a proposal to divide the 9.67-acre site into 3 parcels (with the proposed gas station being located on a 4.24 acre parcel).

The site is located within Airport Compatibility Zone D of the Jacqueline Cochran Regional Airport Influence Area (AIA). Within Compatibility Zone D of the Jacqueline Cochran Regional Airport Land Use Compatibility Plan, non-residential intensity is restricted to an average of 100 people per acre and 300 people in any given single-acre area. The proposed gas station facility includes a 5,275 square foot convenience store and 12 fueling stations, accommodating 106 people. This results in an average intensity for the entire 9.67-acre area of 11 people per acre, and an average intensity on the proposed 4.24-acre parcel of 25 people per acre, both of which would be consistent with the Compatibility Zone D average acre intensity criterion of 100 people. The most intense single-acre area would include the 5,275 square foot convenience store and 12 fueling stations, accommodating 106 people, which would be consistent with the Compatibility Zone D single acre intensity criterion of 300 people.

The elevation at the northwesterly end of Runway 12-30 at Jacqueline Cochran Regional Airport is -117 feet mean sea level (MSL). At a distance of 5,838 feet from the runway to the project, Federal Aviation Administration Obstruction Evaluation Services (FAA OES) review would be required for any structures with a top of roof exceeding -58.62 feet mean sea level. The site’s elevation is -96 feet MSL. The proposed building has a height of 26 feet, resulting in a top point elevation of -70 feet MSL. Therefore, FAA obstruction evaluation review for height/elevation was not required.

AIRPORT LAND USE COMMISSION

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2005 Jacqueline Cochran Regional Airport Land Use Compatibility Plan, as amended in 2006, provided that the County of Riverside applies the following recommended conditions:

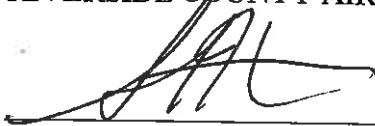
1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The review of this Plot Plan is based on the proposed uses and activities noted in the project description. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use or activity which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use or activity which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use or activity which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area.
 - (d) Any use or activity which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The following uses/activities are specifically prohibited: wastewater management facilities; trash transfer stations that are open on one or more sides; recycling centers containing putrescible wastes; incinerators.
4. The attached notice shall be provided to all potential purchasers of the property and tenants/lessees of the buildings thereon, and shall be recorded as a deed notice.
5. Any new aboveground detention or water quality basins on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention/water quality basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
6. This project has been evaluated for a gas station facility consisting of 12 fueling stations and 5,275 square foot convenience store. Any increase in building area, change in use or modification of the tentative parcel map lot lines and areas will require an amended review by the Airport Land Use Commission.

AIRPORT LAND USE COMMISSION

If you have any questions, please contact Paul Rull, ALUC Urban Regional Planner IV, at (951) 955-6893 or John Guerin, ALUC Principal Planner, at (951) 955-0982.

Sincerely,

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Simon A. Housman, ALUC Director

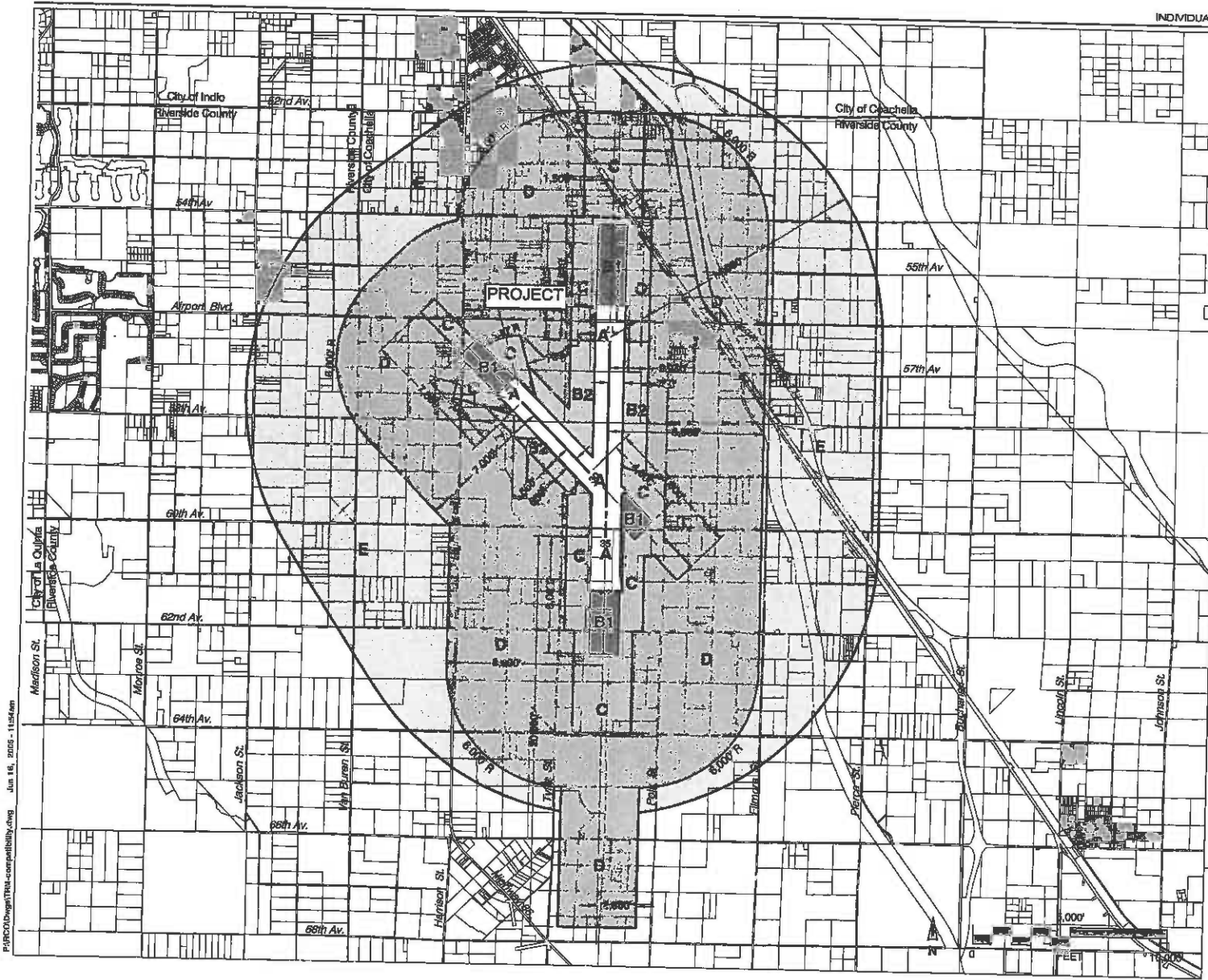
Attachments: Notice of Airport in Vicinity

cc: Tower Energy Group, Mark Vasey (applicant/property owner)
Albert A. Webb Associates, Fayres Hall (representative)
Vince Yzaguirre, Assistant Director, Riverside County Economic Development Agency
Liliana Valle, County Airports Manager
ALUC Case File

Y:\AIRPORT CASE FILES\JCRA\ZAP1043TH18\ZAP1043TH18.LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Legend

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E

Boundary Lines

- Airport Property Line - Existing
- Airport Property Line - Planned
- City Limits

Note

Except for southern extension, Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

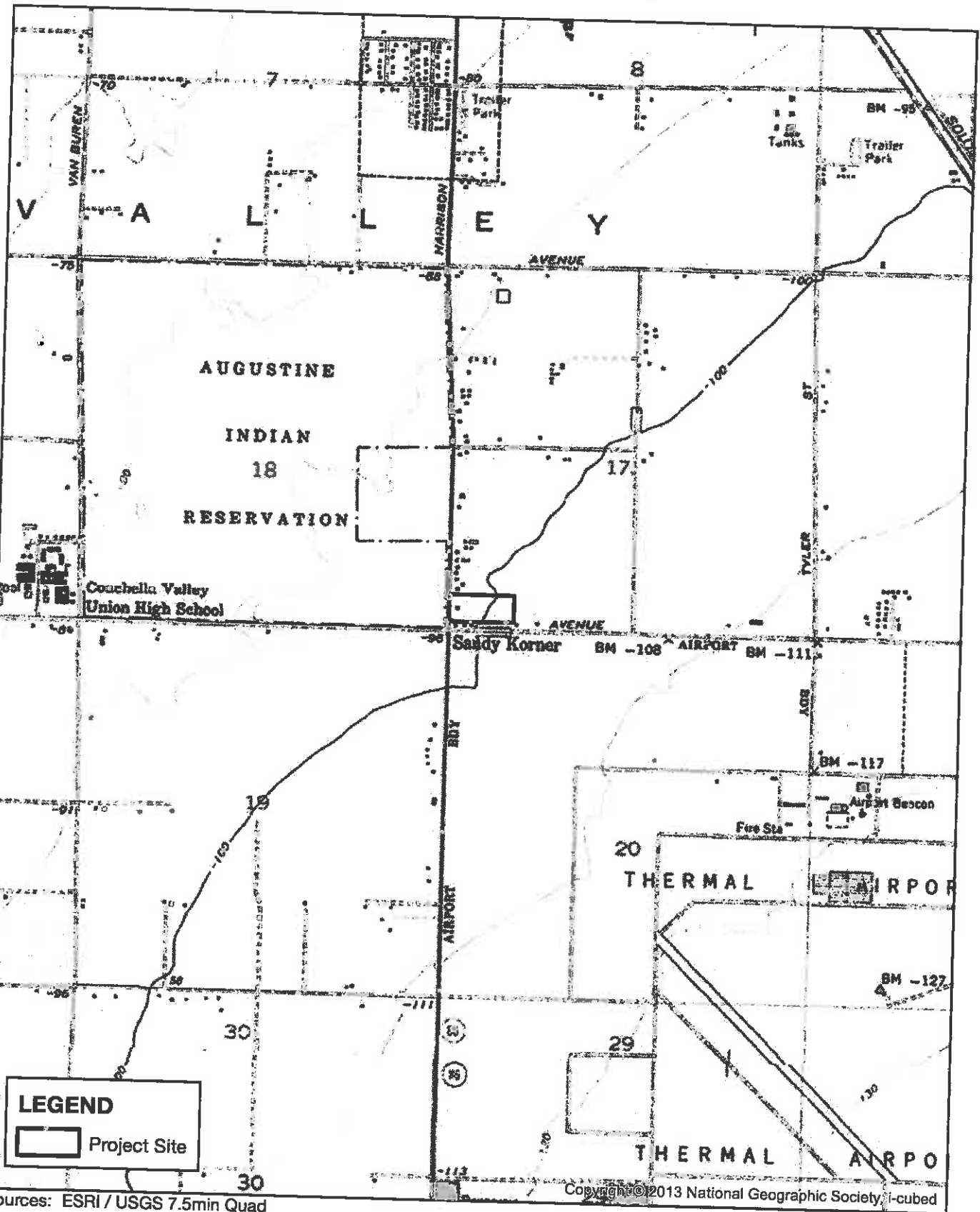
See Chapter 2, Table 2A for compatibility criteria associated with this map.

Riverside County
Airport Land Use Commission
Riverside County
Airport Land Use Compatibility Plan
Policy Document
(Adopted June 2005)

Map JC-1

Compatibility Map
Jacqueline Cochran Regional Airport

G:\2018\18-0748\GIS\USGS.mxd; Map created 10 Aug 2018; nanette

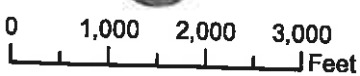


Sources: ESRI / USGS 7.5min Quad DRGs.

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LEGEND

 Project Site

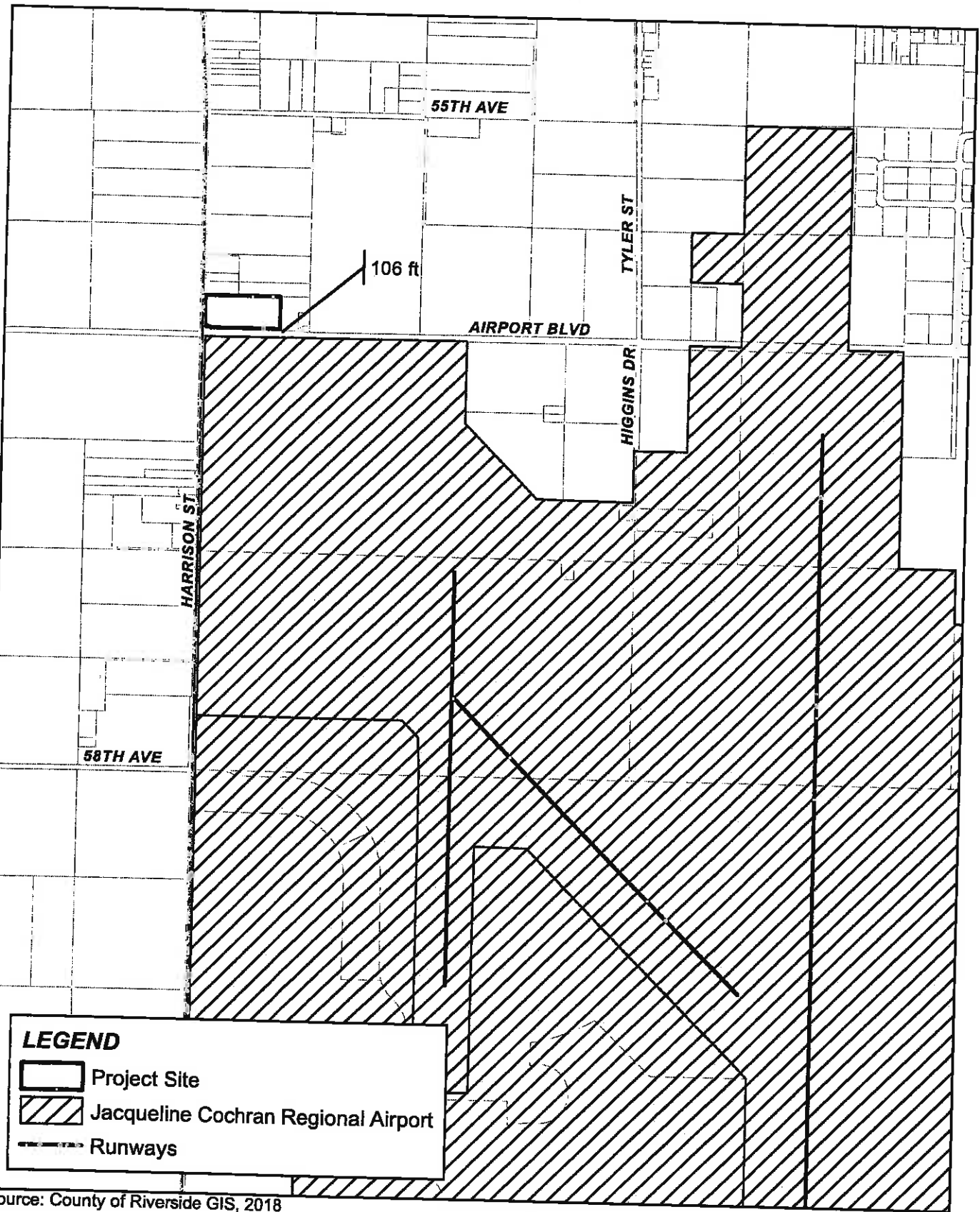


USGS 7.5min Quad: INDIO
Township 6S, Range 8E, Sec. 17




USGS Map
Tower Energy

ALBERT A.
WEBB
ASSOCIATES

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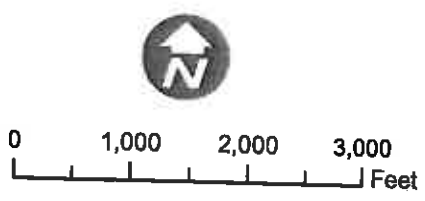


LEGEND

-  Project Site
-  Jacqueline Cochran Regional Airport
-  Runways

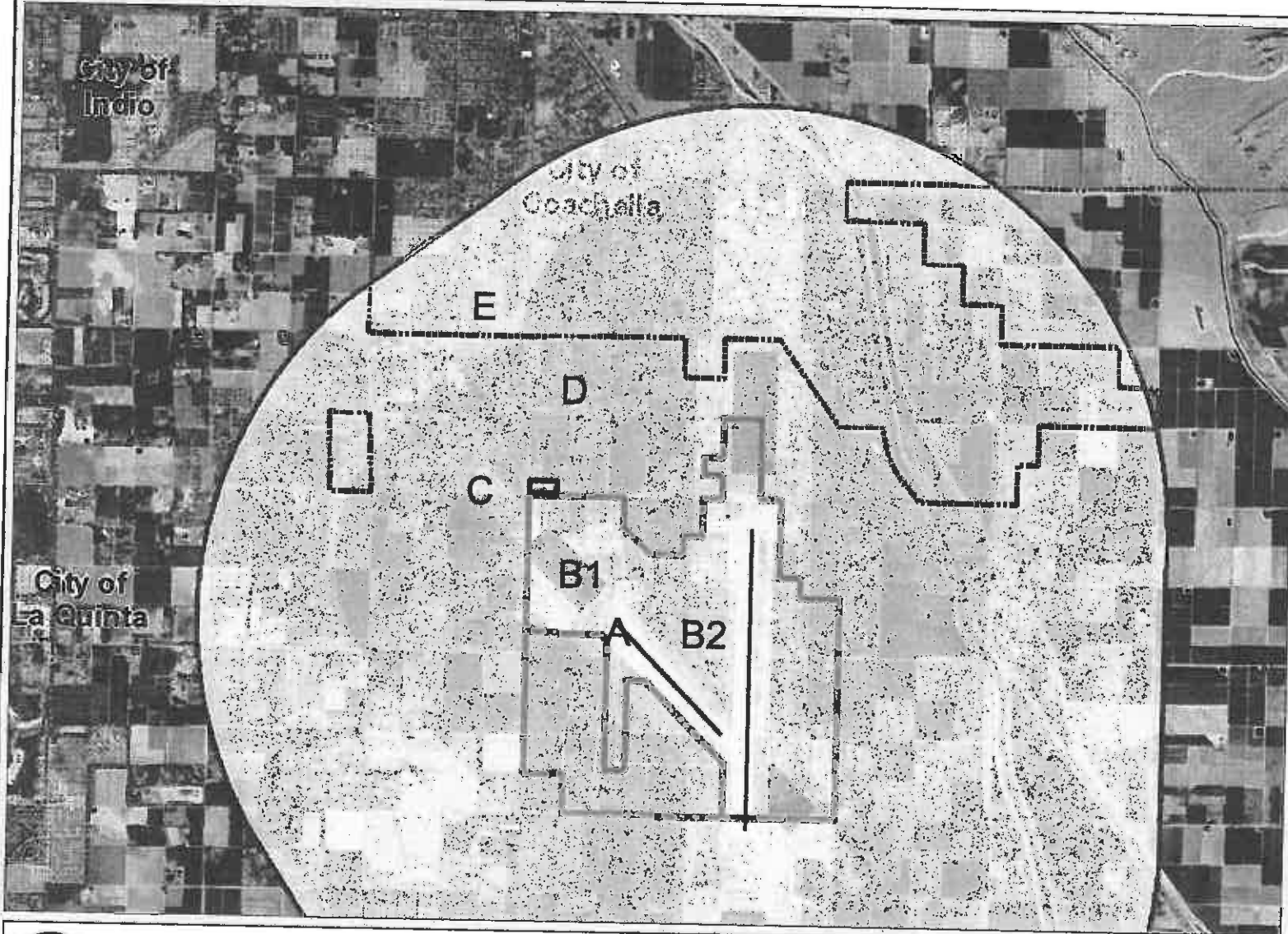
Source: County of Riverside GIS, 2018

Relation to Jaqueline Cochran Regional Airport



Tower Energy
ALBERT A.
WEBB
ASSOCIATES

Map My County Map



Legend

- Runways
- ▣ Airports
- ▣ Airport Influence Areas
- Airport Compatibility Zones
 - ▨ OTHER COMPATIBILITY ZONE
 - A
 - A-EXC1
 - B1
 - B1-APZ I
 - B1-APZ I-EXC1
 - B1-APZ II
 - B1-APZ II-EXC1
 - B1-EXC1
 - B2
 - B2-EXC1
 - C
 - C1
 - C1-EXC1
 - C1-EXC3
 - C1-EXC4
 - C1-HIGHT
 - C2
 - C2-EXC1
 - C2-EXC2
 - C2-EXC3
 - C2-EXC5
 - C2-EXC6



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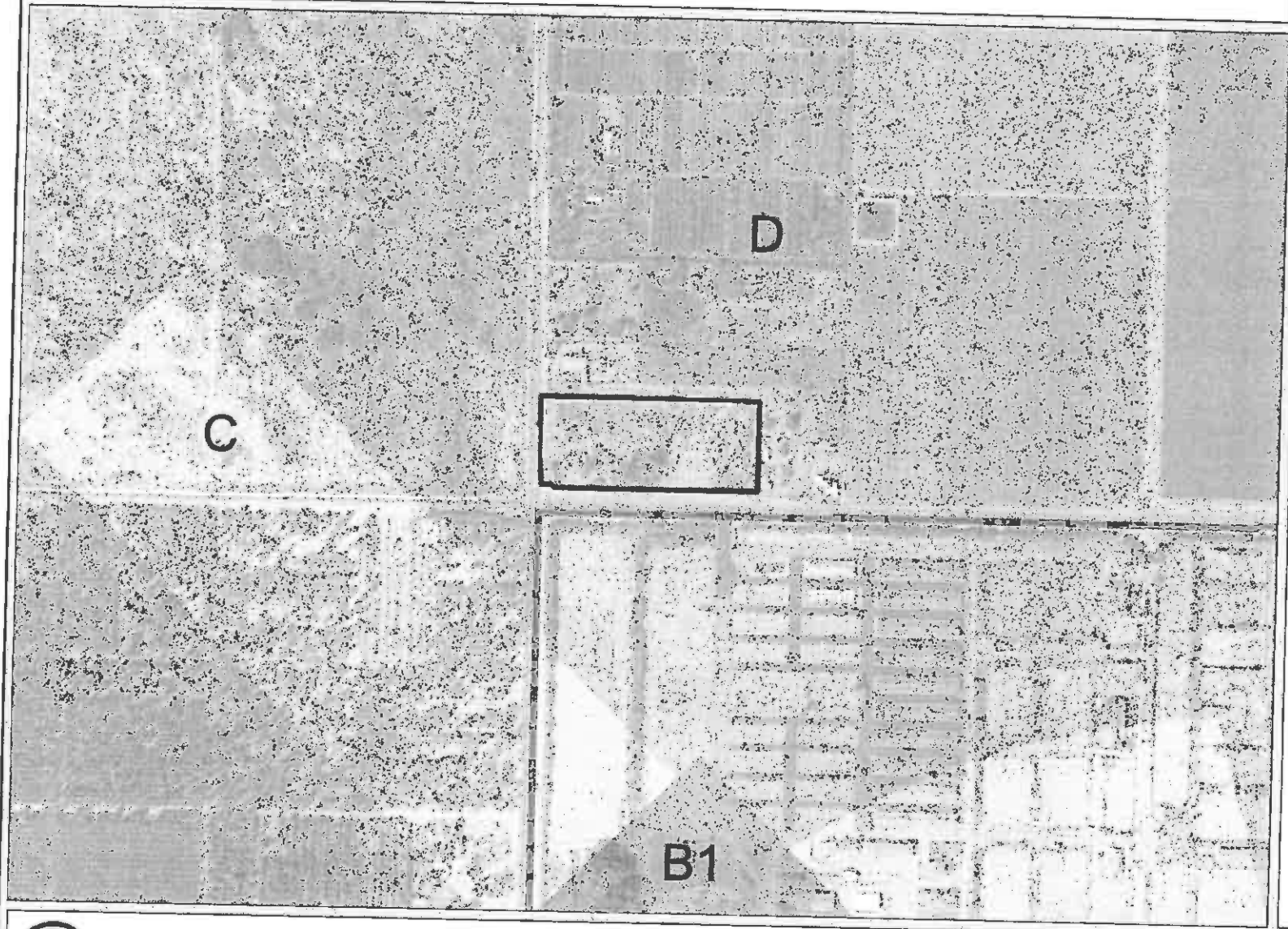


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Notes

Map My County Map



- ### Legend
- Runways
 - Airports
 - Airport Influence Areas
 - Airport Compatibility Zones**
 - ▨ OTHER COMPATIBILITY ZONE
 - A
 - A-EXC1
 - B1
 - B1-APZ I
 - B1-APZ I-EXC1
 - B1-APZ II
 - B1-APZ II-EXC1
 - B1-EXC1
 - B2
 - B2-EXC1
 - C
 - C1
 - C1-EXC1
 - C1-EXC3
 - C1-EXC4
 - C1-HIGHT
 - C2
 - C2-EXC1
 - C2-EXC2
 - C2-EXC3
 - C2-EXC5
 - C2-EXC6



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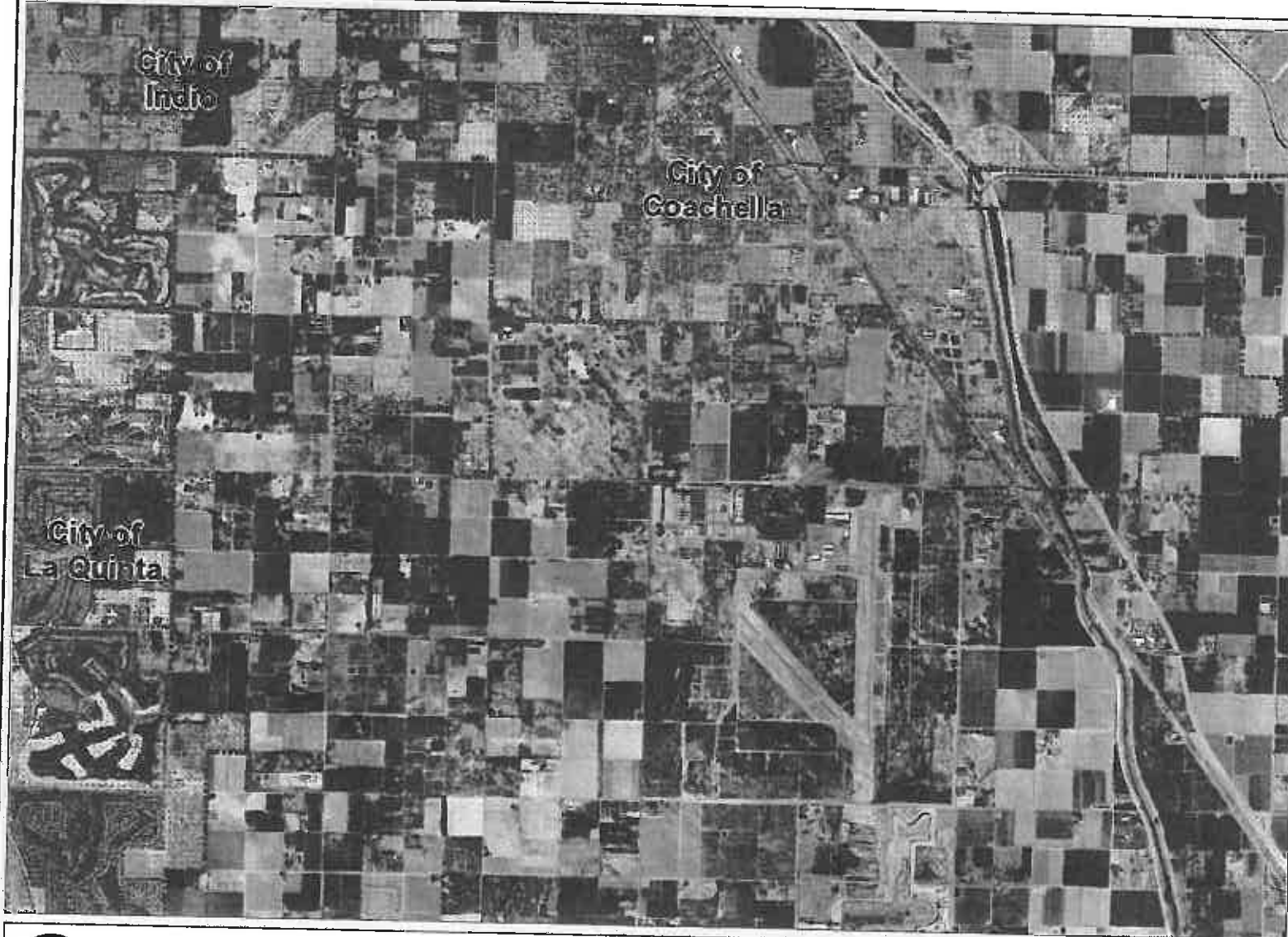


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Notes

Map My County Map



Legend

- Blue line Streams
- City Areas
- World Street Map

Notes



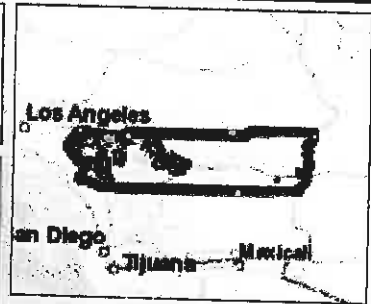
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Map My County Map



Legend

- Blueline Streams
- ▣ City Areas
- World Street Map

Notes



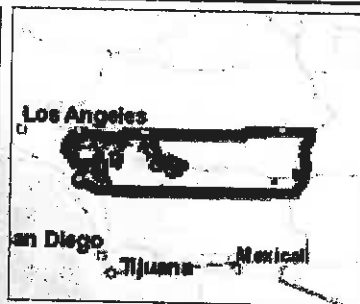
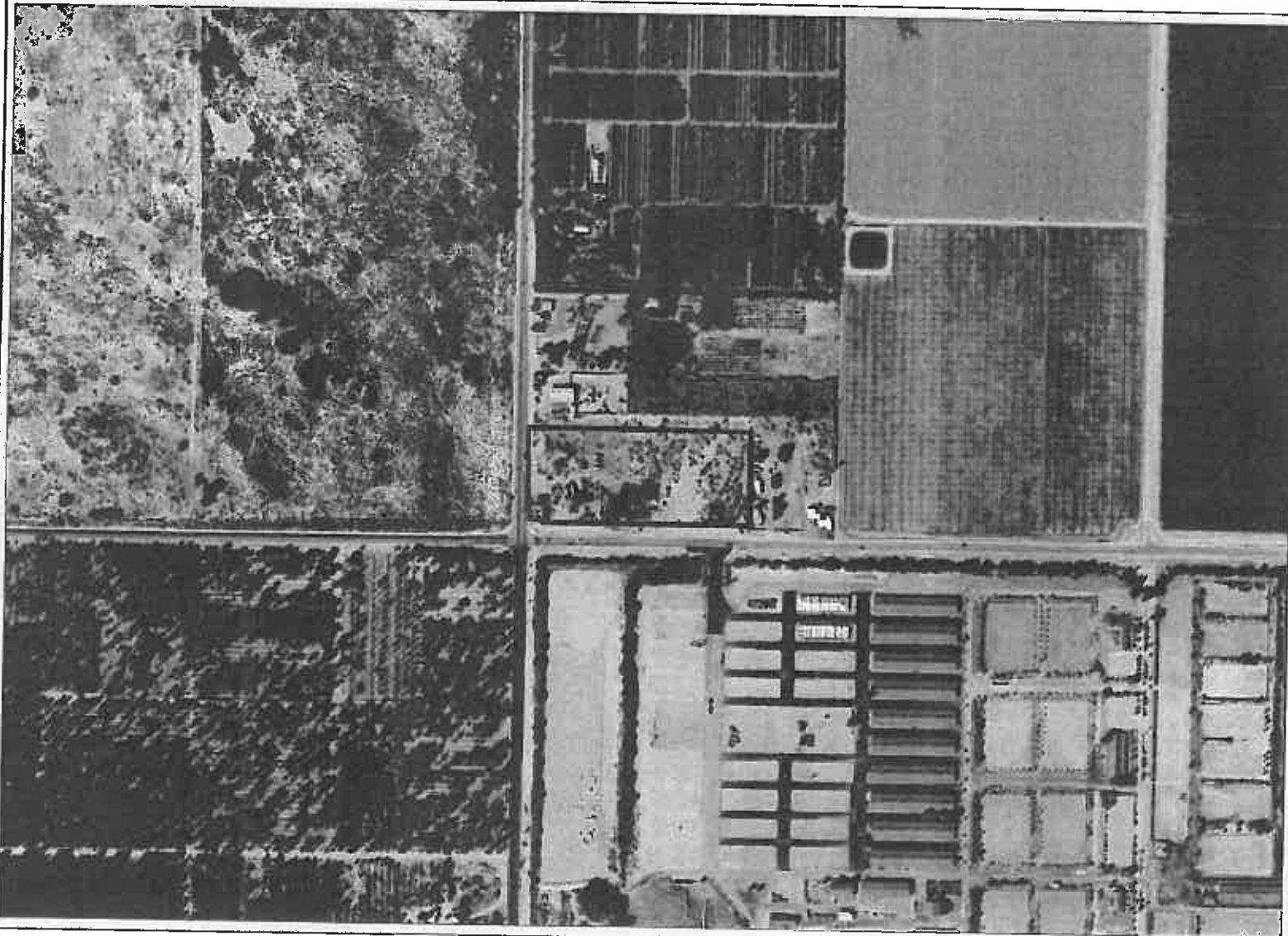
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.



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Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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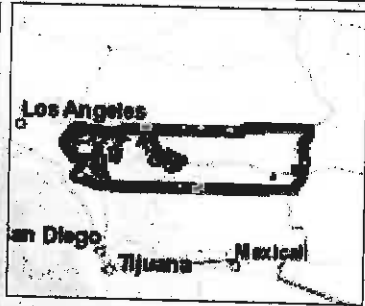
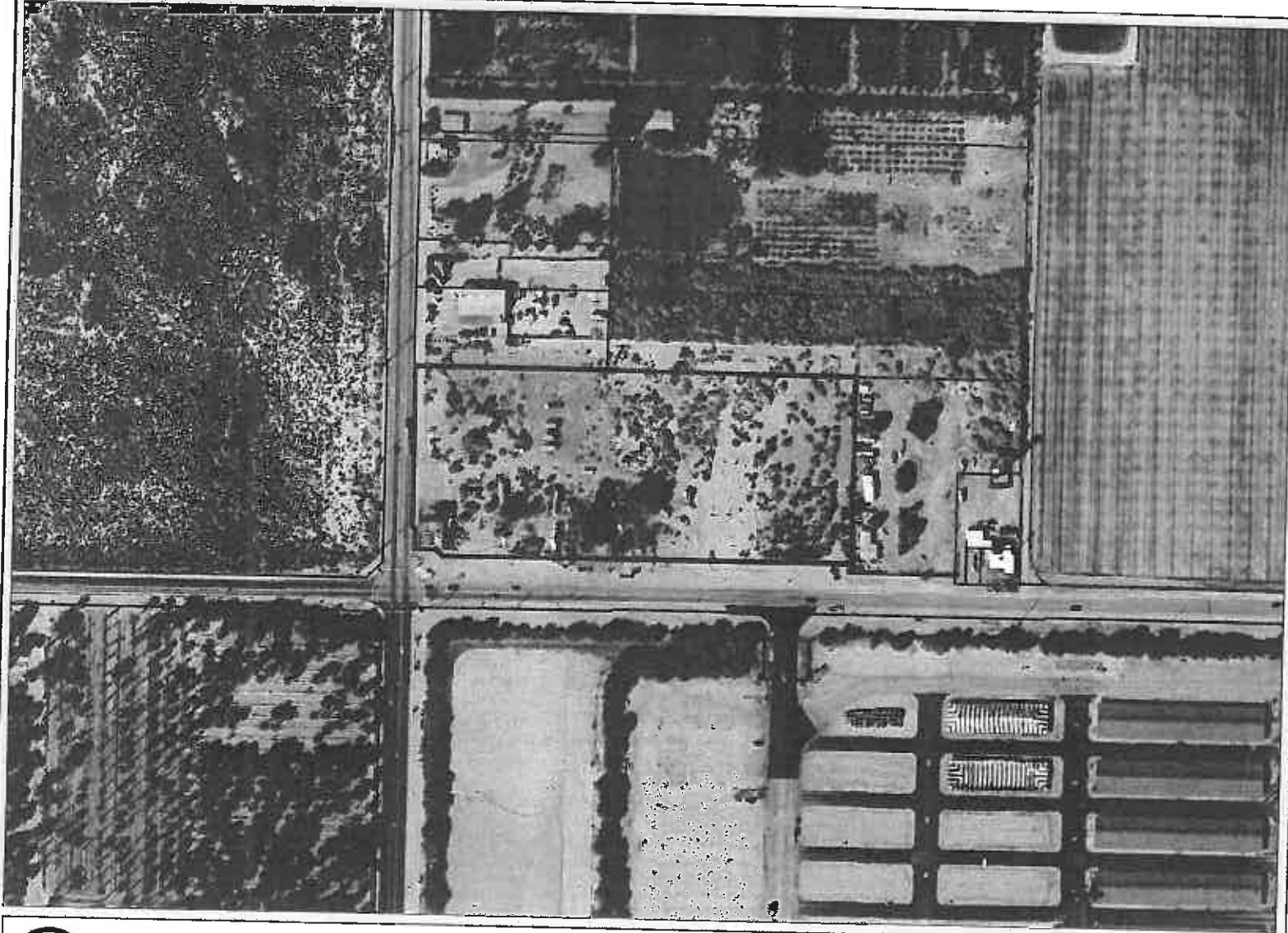
0 770 1,539 Feet

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Notes

Map My County Map



Legend

-  Parcels
-  Blueline Streams
-  City Areas
-  World Street Map

Notes



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IN THE COUNTY OF RIVERSIDE TOWER MARKET CONDITIONAL USE PERMIT NO.

TOPOGRAPHY.
EXISTING TOPOGRAPHY COMPILED BY AN AERIAL SURVEY
ON 07-23-2010 BY ISLAND AERIAL SURVEYS, INC.

SCHOOL DISTRICT.
COANGELLA VALLEY UNIFIED SCHOOL DISTRICT

UTILITY COMPANIES.

- WATER:** COANGELLA VALLEY WATER DISTRICT
2800 HOWLEY LANE EAST
PALM BEACH, CA 92261
PHONE: (760) 938-2661
- SEWER:** COANGELLA VALLEY WATER DISTRICT
2800 HOWLEY LANE EAST
PALM BEACH, CA 92261
PHONE: (760) 938-2661
- ELECTRIC:** SOUTHERN CALIFORNIA Edison COMPANY
221 E FIRST STREET
REARSONT, CA 92528
PHONE: (951) 840-1000
- TELEPHONE:** FRONTIER COMMUNICATIONS
116 4TH STREET
REOLANDS, CA 92875
- WATER:** SOUTHERN CALIFORNIA GAS COMPANY
THE LUDWIG AVE
REOLANDS, CA 92514
PHONE: (909) 938-1700
- CABLE:** CHARTER COMMUNICATIONS
6025 BEAUFORT DRIVE
GARDEN GROVE, CA 92641
PHONE: (760) 974-0000

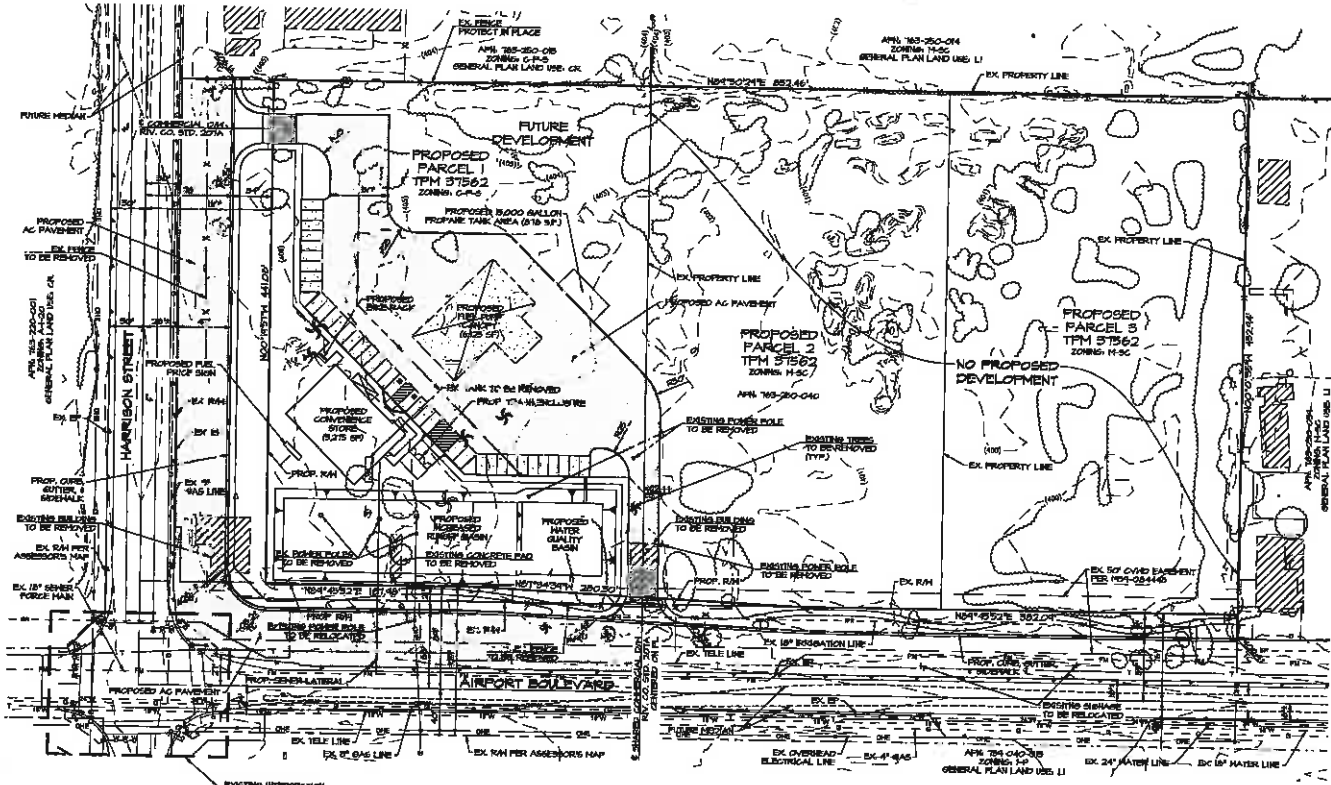
- NOTES**
1. 2.00' INDICATES RATE OF GRADE.
 2. THIS PROJECT IS WITHIN THE EASTERN COANGELLA VALLEY PLANNING AREA.
 3. SIGN PROGRAM WILL BE PROCESSED SEPARATELY.
 4. A LIGHTING PLAN WILL BE SUBMITTED AT A LATER DATE.
 5. ALL PALM TREES TO BE REMOVED OR RELOCATED PER CONCEPTUAL LANDSCAPE PLANS.
 6. TENTATIVE PARCEL MAP 57562 TO BE PROCESSED CONCURRENTLY WITH CUP.
 7. THIS PROJECT IS SUBJECT TO HIGH LIKELIHOOD.
 8. FEMA COMMUNITY PANEL NO. 080603030N, ZONE X, COUNTY OF RIVERSIDE.
 9. LOCATED WITHIN CITY OF COANGELLA SPHERE OF INFLUENCE.
 10. THIS PROJECT IS LOCATED WITHIN JAGGUELINE COCHRAN AIRPORT COMPATIBILITY ZONE D.

LEGAL DESCRIPTION.
THE SOUTHWEST CORNER OF LOT 18 OF COANGELLA LAND AND WATER COMPANY'S SUBDIVISION OF SECTION 11, TOWNSHIP 6 SOUTH, RANGE 8 EAST, AS SHOWN BY MAP OF SAID SUBDIVISION ON FILE IN BOOK 4 PAGE 10 OF MAPS RECORDS OF RIVERSIDE COUNTY, CALIFORNIA.

PROJECT DESCRIPTION.
PROPOSED CONVENIENCE STORE AND FUEL STATION ON A 3.98 ACRES LCA. ADDITIONAL TO BE SUBMITTED PER RIVERSIDE COUNTY STANDARDS.

LAND USE/ZONING

CUP EXISTING LAND USE: COMMERCIAL RETAIL (CR)
CUP PROPOSED LAND USE: COMMERCIAL RETAIL (CR)
CUP EXISTING ZONING: C-P-6
CUP PROPOSED ZONING: C-P-6



ADJACENT PARCEL NO.
785-260-040

APPLICATION TYPE
CUP PER 5.275 OF CONVENIENCE STORE, GAS STATION, FUEL ISLANDS, AND PROPANE TANK.

OWNER: TOWER REAL ESTATE HOLDINGS, LLC
CONTACT: MARK VASEY
ADDRESS: 808 N NORTH STREET, NO. 100
TORRANCE, CA 90504-6284
PHONE: (562) 958-6000

APPLICANT: TOWER ENERGY GROUP
CONTACT: MARK VASEY
ADDRESS: 808 N NORTH STREET, NO. 100
TORRANCE, CA 90504-6284
PHONE: (562) 958-6000

APPLICANT REPRESENTATIVE: ALBERT A. REIS ASSOCIATES
CONTACT: SARA CHANZLER
ADDRESS: 3740 MIDWAY ST
RIVERSIDE, CA 92504
PHONE: (951) 506-1210
FAX: (951) 506-1250

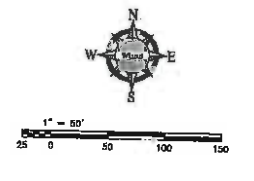
PROJECT DATA

PERMITTED AREA	5076 SQ. FT.
CONCRETE SIDEWALK	3279 SQ. FT.
TOTAL AREA	8355 SQ. FT.
PARKING REQUIREMENTS	71 STALLS
TOTAL PARKING PROVIDED	71 STALLS
ASPHALT PAVED	0
STANDARD	58 STALLS
ADDITIONAL	13 STALLS
TOTAL	71 STALLS

LEGEND

- PROPOSED ASPHALT CONCRETE
- PROPOSED CONCRETE PAVEMENT
- GRASSBREAK/BUCKLELINE
- ADA PATH
- EXISTING CONTOURS
- PROPOSED CONTOURS
- PROPOSED ON-AIR LIGHT FIXTURE
- PROPOSED STREET LIGHT
- PROPOSED FIRE HYDRANT
- DRIVEWAY
- STREET LIGHT
- BENCHMARK
- FIRE HYDRANT
- FLOW LINE
- GRADE BREAK
- BENCH
- SWATH
- LANDSCAPE AREA
- LION POINT
- HAZARD
- PROPERTY LINE
- BOUNDARY
- RIGHT OF WAY
- TYPICAL
- EDGE OF PAVEMENT
- PROPERTY LINE

CRS	DEDICATION	NET
CUP	0.24 AC	0.26 AC
REMANDER	3.48 AC	3.42 AC
TOTAL	3.72 AC	3.68 AC



CRS	DEDICATION	NET
CUP	0.24 AC	0.26 AC
REMANDER	3.48 AC	3.42 AC
TOTAL	3.72 AC	3.68 AC

REVISION	DATE	BY

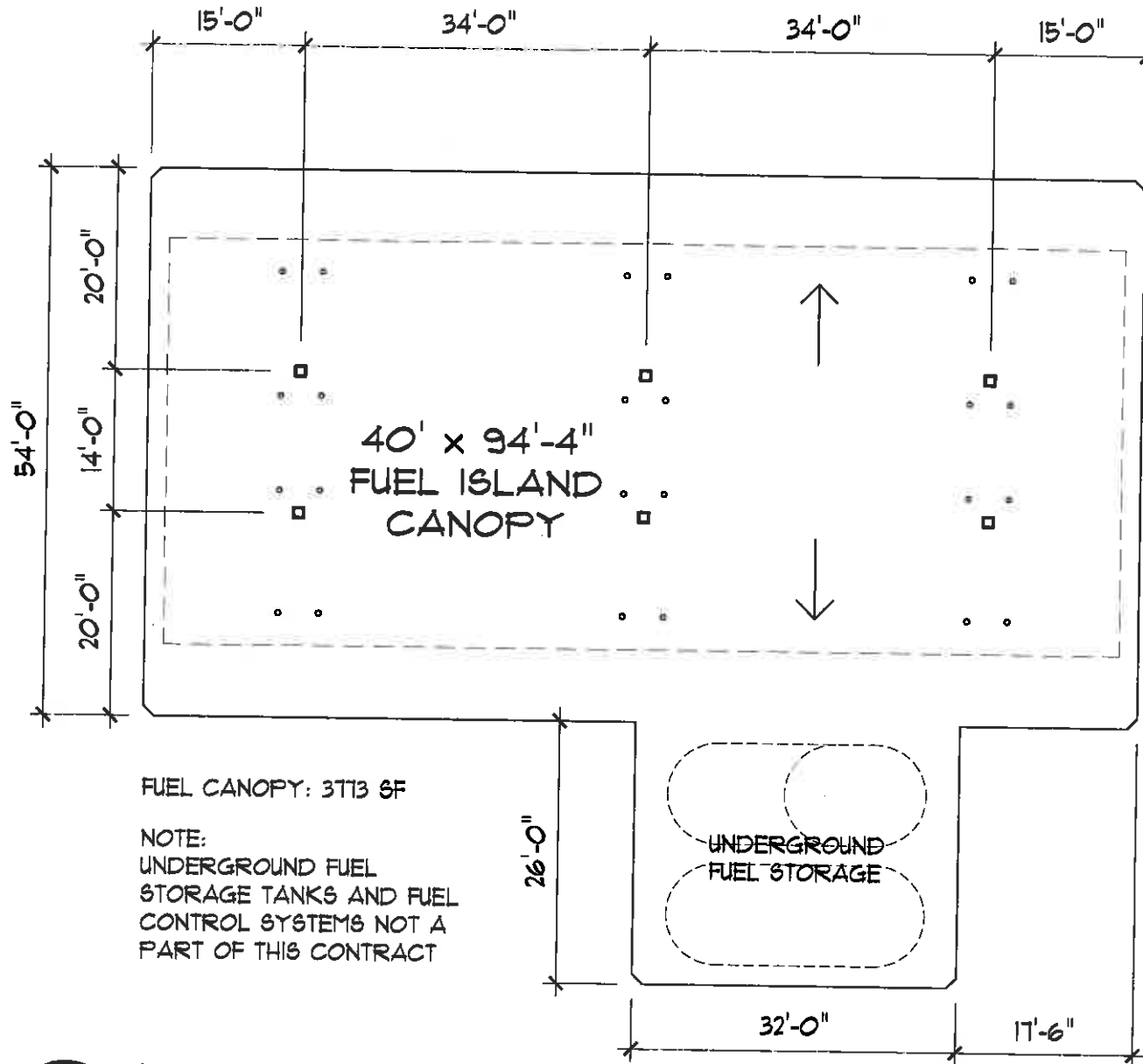
CONDITIONAL USE PERMIT NO.

**COUNTY OF RIVERSIDE
TOWER MARKET
SECTION 11, TOWNSHIP 6 SOUTH, RANGE 8 EAST**

SCALE: 1"=50'
DATE: 6/18/2010
DRAWN: EA
CHECKED: A
PLN. OR. RES.: CA

ADDRESS: 3740 MIDWAY STREET
RIVERSIDE, CA 92504
PH: (951) 506-1210
FAX: (951) 506-1250

NO. 2028-048
SHEET 1 OF 1
DATE: 6/18/2010
DRAWN NO.



1

FUEL CANOPY PLAN

SCALE: 1/16" = 1'-0"

PAGE BREAK





MARCH AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) FUTURE TIMELINE ESTIMATES October 11, 2018

4.2

TASK	2018												2019												2020												2021												2022												2023											
	J	F	M	A	M	J	J	A	S	O	N	D	J	F	M	A	M	J	J	A	S	O	N	D	J	F	M	A	M	J	J	A	S	O	N	D	J	F	M	A	M	J	J	A	S	O	N	D	J	F	M	A	M	J	J	A	S	O	N	D	J	F	M	A	M	J	J	A	S	O	N	D
ALUCP + MND																																																																								
ALUCP + AMEND EIR																																																																								
OFFICE OF ECONOMIC ADJUSTMENT (OEA) GRANT APPLICATION																																																																								
JOINT LAND USE STUDY PROCESS (JLUS)																																																																								
ALUCP & EIR																																																																								

Without Joint Land Use Study (JLUS)
 Airport Land Use Compatibility Plan (ALUCP) and Mitigated Negative Declaration (MND)
 Airport Land Use Compatibility Plan (ALUCP) and Amend Environmental Impact Report (EIR)

Joint Land Use Study Process

1. The optional DOD glare tool has been deleted (October 2018)

PAGE BREAK





4.3

**NOTICE OF AVAILABILITY OF PROPOSED THRESHOLD OF SIGNIFICANCE
CONCERNING NIGHTTIME AIRCRAFT OPERATIONS
AT THE MARCH INLAND PORT AIRPORT**

NOTICE IS HEREBY GIVEN that the March Joint Powers Authority, the March Inland Port Airport Authority, the Successor Agency of the Former March Joint Powers Redevelopment Agency, and the March Joint Powers Utilities Authority (collectively, the “Authority”) seeks to develop a threshold of significance under the California Environmental Quality Act (“CEQA”), pursuant to State CEQA Guidelines section 15064.7. The proposed threshold would address a proposed project’s potential to result in nighttime aircraft operations at the March Inland Port Airport that could expose people to a significant risk of sleep disturbance due to noise. The proposed threshold of significance being considered for adoption would provide:

Noise. Would the proposed project result in aircraft operations (i.e., aircraft landings and/or takeoffs) at the March Inland Port Airport between 10:00 p.m. and 6:59 a.m. that could expose people within the March Inland Port Airport’s vicinity to a significant risk of sleep disturbance due to noise?

If adopted, this threshold of significance would apply to future proposed CEQA projects that require discretionary approval from any of the Authority.

Public Review and Comment: The Authority seeks to develop the proposed threshold of significance through a public review process, and encourages interested persons to submit public comments regarding the proposed threshold. Interested persons can review the proposed threshold and supporting information at www.marchjpa.com and at the March Joint Powers Authority’s office located at 14205 Meridian Parkway, Suite 140, Riverside, CA 92518.

A public review and comment period of at least 30 days is being provided to accept comments regarding the proposed threshold. The comment period will conclude on October 18, 2018 at 5:00 p.m. Please submit any written comments to:

Carey Allen, March Joint Powers Authority Clerk
14205 Meridian Parkway, Suite 140
Riverside, CA 92518

Informational Meeting: An informational meeting to accept comments from the public will be held at 3:00 p.m. on September 24, 2018 at Western Municipal Water District, Board Room, 14205 Meridian Parkway, Riverside, CA 92518 to provide the public an additional opportunity to participate in the public review of the proposed threshold.

BACKGROUND AND SUMMARY OF PROPOSED CEQA THRESHOLD OF SIGNIFICANCE CONCERNING NIGHTTIME AIRCRAFT OPERATIONS AT THE MARCH INLAND PORT AIRPORT

Who are we?

The March Joint Powers Authority (“March JPA”) is a joint powers agency created by a joint powers agreement to act as the federally recognized reuse authority, local land use authority, redevelopment agency, and airport authority for the former March Air Force Base. The March Inland Port Airport Authority (“MIPAA”) was formed to operate, manage, and promote the civilian aviation activities at the March Air Reserve Base in Riverside County, California (“Airport”).

The March JPA and MIPAA – along with related entities including the Successor Agency to the Former March Joint Powers Redevelopment Agency, and the March Joint Powers Utilities Authority – (collectively, the “Authority”) are concerned that potential future CEQA projects may propose aircraft operations (i.e., aircraft landings and/or takeoffs) at the Airport between 10:00 p.m. and 6:59 a.m. that could expose people within the Airport’s vicinity to a significant risk of sleep disturbance due to noise. Accordingly, the Authority seeks to develop and publish a threshold of significance to ensure that any such environmental impacts are identified, analyzed, and fully disclosed under the California Environmental Quality Act (“CEQA”).

What is a CEQA threshold of significance?

A CEQA “project” is the whole of an action, which has the potential for resulting in a direct or reasonably foreseeable indirect impact on the physical environment, and which requires a discretionary approval from a California public agency. (See State CEQA Guidelines,¹ § 15378.) CEQA generally requires that an environmental impact report (“EIR”) be prepared for any such proposed “project,” if the project “may have a *significant effect* on the environment.” (Pub. Resources Code, § 21151 [emphasis added].)

To determine whether a proposed project may have a significant effect on the environment, the State CEQA Guidelines encourage each public agency “to develop and publish thresholds of significance that the agency uses in the determination of the significance of environmental effects.” (State CEQA Guidelines, § 15064.7(a).) “A threshold of significance is an identifiable quantitative, qualitative, or performance level of a particular environmental effect, non-compliance with which means the effect will normally be determined to be significant by the agency and compliance with which means the effect normally will be determined to be less than significant.” (State CEQA Guidelines, § 15064.7(a).)

What threshold of significance is being proposed today?

To address the Authority’s concern about the potential of proposed projects to result in nighttime aircraft operations that could expose people to a significant risk of sleep disturbance due to noise,

¹ The State CEQA Guidelines are codified in Title 14 of the California Code of Regulations, commencing at section 15000.

the Authority proposes the following threshold of significance for use as part of the Authority's future environmental review processes under CEQA:

Noise. Would the proposed project result in aircraft operations (i.e., aircraft landings and/or takeoffs) at the March Inland Port Airport between 10:00 p.m. and 6:59 a.m. that could expose people within the March Inland Port Airport's vicinity to a significant risk of sleep disturbance due to noise?

This proposed threshold, if adopted, would be implemented as part of the Authority's Local Guidelines for Implementing the California Environmental Quality Act. Specifically, any Initial Study prepared for a proposed project would consider and address the above threshold of significance, in addition to the other questions presented in the Appendix G Initial Study checklist found in the State CEQA Guidelines. If the Authority answers the threshold question in the affirmative, it would normally be determined that the proposed project would have a significant impact on the environment, and an EIR would normally be required pursuant to CEQA.

Why develop this threshold?

There are currently no accepted state or federal thresholds for evaluating impacts specifically associated with awakenings and sleep disturbance due to nighttime aircraft operations. (June 22, 2018 Noise Technical Memorandum ("Noise Technical Memorandum"), p. 8.) The State CEQA Guidelines, however, nonetheless encourage public agencies to develop and publish their own thresholds of significance, as noted above.

Further, the Authority has received many public comments and concerns over the years, stating that the noise generated by the operation of nighttime flights creates sleep disturbances and awakenings in a manner which the community believes is a significant impact. As such, the Authority is proposing this threshold and circulating it for public review in response to those community concerns.

More specifically, the Authority has proposed the above-referenced threshold of significance based on substantial evidence. Notably:

- Research has indicated that sleep disturbance caused by noise generated by nighttime aircraft operations, including nighttime awakenings, may have deleterious impacts on both mental and physical health. Health impacts may qualify as a potential environmental impact under CEQA. (See *Berkeley Keep Jets Over The Bay Committee v. Board of Port Com'rs* (2001) 91 Cal.App.4th 1344, 1372 [hereinafter, "*Berkeley*"] [proposed project's "potential interference with sleep, including physiological response and annoyance from increased nighttime overflights" may constitute a potential "significant effect on the environment" that must be studied in an EIR under CEQA].)
- The Authority has received written and oral comments from community members stating that nighttime aircraft operations at the Airport have adversely affected their sleep. (*Berkeley, supra*, 91 Cal.App.4th at pp. 1380-1381 ["citizens' personal observations about

the significance of noise impacts on their community constituted substantial evidence that the impact may be significant and should be assessed in an EIR, even though the noise levels did not exceed general planning standards”] citing *Oro Fino Gold Mining Corp v. County of El Dorado* (1990) 225 Cal.App.3d 872, 882.)

- Aircraft operations at the Airport may cause a percentage of the surrounding population to experience nighttime awakenings due to aircraft noise. (Noise Technical Memorandum, p. 10, Figures 7 & 8.) The Noise Technical Memorandum reached this conclusion employing a noise descriptor known as the A-weighted Sound Exposure Level (“SEL”), which is a time integrated measure, expressed in decibels, of the sound energy of a single noise event at a reference duration of one second. SEL accounts for both the maximum sound level and the duration of the sound. However, SEL is not the only available noise descriptor. The Community Noise Equivalent Level (“CNEL”) noise descriptor is a method of *averaging* a 24-hour sound level, in decibels, obtained from the accumulation of all sound sources. The CNEL calculates the total sound exposure, in decibels, at a given location and then divides the total by 24 hours to derive an average. Sound levels occurring at night from 10:00 p.m. to 6:59 a.m. are multiplied by 10 resulting in a 10 dB penalty, and sound levels occurring in the evening from 7:00 p.m. to 9:59 p.m. are multiplied by 3 resulting in a 4.77 dB penalty.
- The Noise Technical Memorandum noted that the State of California uses a CNEL contour of 65 dBA for determining aircraft noise impacts on residential land uses and confirming compatibility with residential land uses.² (Noise Technical Memorandum, p. 8.) However, the 65 dBA CNEL standard for determining *land-use* compatibility is not necessarily determinative of what nighttime *noise* levels (and sleep disturbances or awakenings resulting from that noise) are acceptable for the residents near the March Inland Port Airport.
- The Authority is considering whether the use of the SEL noise metric (in addition to the use of the CNEL noise indicator) is a more appropriate measure to properly capture a proposed project’s nighttime noise impact when considering sleep disturbances and awakenings. Because the SEL is a single-event noise metric, it may better capture the potential for sleep disturbances than the CNEL.
- The proposed threshold concerns nighttime noise generated by aircraft operations between 10:00 p.m. and 6:59 a.m. because noise has increased effects on sleep patterns during these hours. This is reflected in the California Airport Noise Standards.³ The emphasis on minimizing nighttime noise in residential areas is also generally consistent with the municipal code sections concerning noise levels for the local land use jurisdictions surrounding the Airport – i.e., the County of Riverside, the City of Riverside, the City of

² The Noise Technical Memorandum noted that using a conservative CNEL of 60 dBA would not capture any residential land uses surrounding the Airport. (Noise Technical Memorandum, p. 8.)

³ “Community noise equivalent level, in decibels, represents the average daytime noise level during a 24-hour day, adjusted to an equivalent level to account for the lower tolerance of people to noise during evening and night time periods relative to the daytime period.” (Cal. Code Regs., tit. 21, § 5001(f).)

Perris, and the City of Moreno Valley. Specifically, the municipal codes in Riverside County, the City of Riverside, and the City of Perris all have reduced permissible noise levels for residential areas between 10:00 p.m. and 7:00 a.m., and the City of Moreno Valley reduces the permissible level of noise during “nighttime.”⁴

The Authority invites the public to review and comment upon the proposed threshold of significance. Copies of the Notice of Availability, this Summary Memo, the Noise Technical Memorandum referenced herein, and other supporting information is located at the March Joint Powers Authority’s office, located at 14205 Meridian Parkway, Suite 140, Riverside, CA 92518 and also on the internet at www.marchjpa.com.

⁴ See County of Riverside Municipal Code, § 9.52.040; City of Riverside Municipal Code, §§ 7.25.010, 7.30.015; City of Perris Municipal Code, §§ 7.34.040, 7.34.050; City of Moreno Valley Municipal Code, §11.80.030.

**AIRPORT LAND USE COMMISSION
MINUTE ORDER SEPTEMBER 13, 2018
RIVERSIDE MEETING**

A regular scheduled meeting of the Airport Land Use Commission was held on September 13, 2018 at the Riverside County Administrative Center, Board Chambers.

COMMISSIONERS PRESENT: Richard Stewart, Acting Chairman
Arthur Butler
Steven Stewart
Gary Youmans

COMMISSIONERS ABSENT: Steve Manos, Chairman
Russell Betts, Vice Chairman
John Lyon

STAFF PRESENT: Simon Housman, ALUC Director
John Guerin, Principal Planner
Paul Rull, Urban Regional Planner IV
Barbara Santos, ALUC Commission Secretary
Raymond Mistica, ALUC Counsel

OTHERS PRESENT: None

**AIRPORT LAND USE COMMISSION
MINUTE ORDER SEPTEMBER 13, 2018
RIVERSIDE MEETING**

I. **AGENDA ITEM 3.1:** ZAP1030BA18 – Copart (Representative: Stantec Consulting Services, Inc.) – City of Banning Planning Case Nos. ZTA18-97503 (Zoning Text Amendment), CUP18-8002 (Conditional Use Permit), DR18-7008 (Design Review), TPM18-4001 (Tentative Parcel Map No. 37487). The applicant proposes to construct a 12,622 square foot office/service building in conjunction with an existing 4,670 square foot building for an online vehicle liquidation business that includes auto auction, sale, and storage of used vehicles on a 63.34 acre portion of a 94.7 acre site located easterly of Hathaway Street, northerly of Interstate 10, and southerly of Morongo Road. The applicant also proposes amending the City’s Zoning Ordinance to allow for parking and storage of vehicles on unpaved surfaces. Also proposed is a parcel map to divide the site into 10 commercial parcels in two phases. Parcels 1 and 2 (phase 1) are part of the proposed vehicle liquidation business. No structures are proposed on Parcels 3 through 10 (phase 2) at this time. Parcel 9 is anticipated for use as a water quality basin. (Airport Compatibility Zone D of the Banning Municipal Airport Influence Area).

II. **MAJOR ISSUES**
None

III. **STAFF RECOMMENDATION**
Staff recommends that the Commission find the proposed Zoning Ordinance Amendment CONSISTENT with the 2004 Banning Airport Land Use Compatibility Plan as amended in 2016, and find the Conditional Use Permit, Design Review, and Tentative Parcel Map CONDITIONALLY CONSISTENT, subject to the conditions included herein, and such additional conditions as may be required by the Federal Aviation Administration Obstruction Evaluation Service.

IV. **PROJECT DESCRIPTION**
The applicant proposes to construct a 12,622 square foot office/service building in conjunction with an existing 4,226 square foot building for an online vehicle liquidation business that includes auction, sale, and short-term storage of used vehicles (including damaged vehicles) on a 63.34 acre portion of a 94.7 acre site. The applicant also proposes amending the City’s Zoning Ordinance to allow for parking and storage of vehicles on unpaved surfaces. Also proposed is a parcel map to divide the full 94.7 acre site into 10 commercial parcels in two phases. Parcels 1 and 2 (phase 1) are part of the proposed vehicle liquidation business. No structures are proposed on Parcels 3 through 10 (phase 2) at this time. Parcel 9 is anticipated for use as a water quality basin.

On March 11, 2010, the Commission previously found conditionally consistent (pending Federal Aviation Administration review) ZAP1006BA10 at this location for a proposal to develop twelve industrial buildings with a total gross floor area of 1,194,045 square feet on a 63.98-64.50 acre site and a parcel map to divide the property into twelve lots, so that each building would be located on its individual lot.

CONDITIONS: Final conditions await FAA approval for the (CUP, Design Review, Parcel Map)

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final

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approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.

- (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, artificial marshes, wastewater management facilities, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached notice shall be provided to all potential purchasers of the property and all potential tenants of the buildings, and shall be recorded as a deed notice.
 4. The proposed detention basins on the site (including water quality management basins) shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
 5. At least 9.47 acres of ALUC-eligible open areas (at least 75 feet in width and 300 feet in length), shall be kept obstacle and obstruction free per ALUC open area definition (no objects greater than four feet in height with a diameter of four inches or greater).
 6. This project has been evaluated as a proposal for 8,198 square feet of office area, 4,623 square feet of warehouse area, 1,187 square feet break room area, and 471 square feet of storage area. Any increase in total building area or change to more intensive uses will require review by the Airport Land Use Commission.

V. MEETING SUMMARY

The following staff presented the subject proposal:

Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

No one spoke in favor, neutral or opposition to the project.

VI. ALUC COMMISSION ACTION

The ALUC by a unanimous vote of 4-0 found the project **CONSISTENT** (Zoning Ordinance Amendment); **CONDITIONALLY CONSISTENT** (CUP, Design Review, Parcel Map) subject to the conditions included herein, and such additional conditions as may be required by the FAA OES. Absent: Commissioners Manos, Betts, Lyon

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VII. CD

The entire discussion of this agenda item can be found on CD and referenced by the meeting time listed below. For a copy of the CD, please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at basantos@rivco.org.

ITEM 3.1: TIME: 9:33 A.M.

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- I. **AGENDA ITEM 3.2:** ZAP1068PS18 – Painted Hills Wind, LLC (Representative: Robert Skaggs) – Riverside County Planning Case Nos. WCS180001 (Commercial WECS Permit) and VAR180003 (Variance). The applicant proposes to decommission and remove approximately 291 existing commercial wind turbines and install 14 new commercial wind turbines with a maximum height of 499 feet with a per turbine energy generating capacity between 2.0 megawatts (MW) and 4.2 MW on 600 acres located northerly of Avenue 16, easterly of Whitewater Canyon Road, westerly of Windhaven Road at the terminus of Painted Hills Road. Also proposed are associated equipment such as a laydown yard, construction of new temporary and permanent internal roads, and new 12-kilovolt (kV) underground/overhead electrical collection lines. A variance is also requested proposing reductions in safety, wind access, and scenic setbacks. (Not located within an Airport Compatibility Zone).
- II. **MAJOR ISSUES**
The project is currently being reviewed by the Federal Aviation Administration Obstruction Evaluation Service for impacts to air navigation. There are significant concerns regarding the extreme height of the proposed wind turbines, as well as electromagnetic interference and/or interference to the airport approach/departure radar surveillance system. The project is being recommended for a continuance until such time that the FAA review is completed.
- III. **STAFF RECOMMENDATION**
Staff recommends CONTINUANCE of the project to the Commission's October 11 meeting, unless Determinations of No Hazard to Air Navigation are issued by September 13.
- IV. **PROJECT DESCRIPTION**
The applicant proposes to decommission and remove approximately 291 existing commercial wind turbines and install 14 new commercial wind turbines with a maximum height of 499 feet with a per turbine energy generating capacity between 2.0 megawatts (MW) and 4.2 MW on 600 acres. Also proposed are associated equipment such as two meteorological towers, laydown yard, construction of new temporary and permanent internal roads, and new 12-kilovolt (kV) underground/overhead electrical collection lines. A variance is also requested proposing reductions in safety, wind access, and scenic setbacks. Wind turbine heights are measured at top of blade in the "twelve o'clock position".
- V. **MEETING SUMMARY**
The following staff presented the subject proposal:
Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org
- No one spoke in favor, neutral or opposition to the project.
- VI. **ALUC COMMISSION ACTION**
The ALUC by a unanimous vote of 4-0 CONTINUED the project to October 11, 2018. Absent: Commissioners Manos, Betts, Lyon
- VII. **CD**
The entire discussion of this agenda item can be found on CD and referenced by the meeting time listed below. For a copy of the CD, please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at basantos@rivco.org.

ITEM 3.2: TIME: 9:39 A.M..

**AIRPORT LAND USE COMMISSION
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RIVERSIDE MEETING**

I. **AGENDA ITEM 3.3:** ZAP1317MA18 – Meridian Park, LLC (Representative: Jeff Gordon) – March Joint Powers Authority Case Nos. GPA18-01 (General Plan Amendment), SPA18-01 (Specific Plan Amendment), TTM18-01 (Revision to Tentative Tract Map No. 30857), PP18-02 and PP18-03 (Plot Plans). PP18-02 proposes a commercial development consisting of 140,570 square feet of office area, 7,905 square foot commercial retail building (which may include dining establishments) with drive-through, a 3,300 square foot fast food restaurant with drive-through, a gas station with 12 vehicle fueling pumps and a 3,062 square foot convenience store on 23.68 acres located southerly of Van Buren Boulevard and easterly of the southerly extension of Orange Terrace Drive. PP18-03 proposes a 500,000 square foot industrial high cube warehouse building on 22.96 acres located on the northeast corner of Krameria Avenue and Coyote Bush Road. The applicant also proposes to amend the March Joint Powers Authority General Plan and March Business Center Specific Plan SP-1 (March Business Center Specific Plan SP-1 Amendment #7) land use designations of 61.93 acres within SP-1 South Campus as follows: 6.57 acres from Commercial to Business Park; 3.7 acres, from Office to Commercial; 4.11 acres from Business Park to Office; 25.77 acres, from Business Park to Industrial; and 21.78 acres, from Industrial to Business Park. The applicant also proposes revisions to the numbers, sizes, and locations of proposed lots in Tentative Tract Map No. 30857. (Airport Compatibility Zone C2 High Terrain Zone of the March Air Reserve Base/Inland Port Airport Influence Area).

II. **MAJOR ISSUES**
None

III. **STAFF RECOMMENDATION**
Staff recommends that the Commission find the proposed General Plan Amendment and Specific Plan Amendment CONSISTENT with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, and find the proposed Tentative Tract Map and Plot Plans CONSISTENT, subject to the conditions included herein.

IV. **PROJECT DESCRIPTION**
PP18-02 proposes a commercial development consisting of 140,570 square feet of office area, 7,905 square foot commercial retail building (which may include dining establishments) with drive-through, a 3,300 square foot fast food restaurant with drive-through, a gas station with 12 vehicle fueling pumps, and a 3,200 square foot convenience store on 23.68 acres (located southerly of Van Buren Boulevard and easterly of the southerly extension of Orange Terrace Drive). PP18-03 proposes a 500,000 square foot industrial high cube warehouse building on 22.96 acres (located on the northeast corner of Krameria Avenue and Coyote Bush Road).

The applicant also proposes to amend the March JPA General Plan and March Business Center Specific Plan land use designations on five sites within the South Campus of the March Business Center as follows:

- (1) to enable development proposed through PP18-02, change 3.7 gross acres (3.49 net acres due to roadway dedications) of land located southerly of Van Buren Boulevard and easterly of the southerly extension of Orange Terrace Drive from Office to Commercial (leaving a remaining 9.42 gross acres designated as Office);
- (2) to enable development proposed through PP18-03, change approximately 25.77 gross acres of land located on the northeast corner of Krameria Avenue and Coyote Bush Road from Business Park to Industrial;
- (3) change approximately 21.78 gross acres located easterly of the PP18-03 site from Industrial to Business Park;
- (4) change 6.57 gross acres (5.84 net acres due to roadway dedications) of land on the west side

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- of Village West Drive, southerly of Van Buren Boulevard from Commercial to Business Park; and
- (5) change approximately 4.11 gross acres (3.65 net acres due to roadway dedications) on the northwest corner of Van Buren Boulevard and Coyote Bush Road from Business Park to Office.

The applicant also proposes amending previously approved, but never recorded, Tentative Tract Map No. 30857 by:

- 1) adjusting the boundaries of tentatively approved Parcels 4 through 7 so as to result in the following acreages – Parcel 4, 22.93 acres; Parcel 5, 10.33 acres; Parcel 6, 9.51 acres; Parcel 7, 15.6 acres;
- 2) reducing Parcel 41 to 3.65 acres, consolidating the southern portion of Parcel 41 and Parcels 49 and 50 with Parcel 48, resulting in a revised acreage of 5.03 acres for Parcel 48; and
- 3) adjusting the boundaries of tentatively approved Parcels 72 through 75 so as to result in the following acreages: Parcel 72, 3.7 acres; Parcel 73, 10.77 acres; Parcel 74, 9.42 acres; and Parcel 75, 8.24 acres.

Parcel 72 becomes the site of retail, dining, and fueling uses proposed through PP18-02, while the office buildings are on Parcels 73 and 74. Revised Parcel 4 becomes the site of the industrial building proposed through PP18-03.

CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the

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operation of aircraft and/or aircraft instrumentation.

- (e) Hazards to flight. Children's schools are discouraged in Compatibility Zone C2.
- 3. Prior to issuance of any building permits, the landowner shall convey and have recorded an aviation easement to the March Inland Port Airport Authority. Contact March Joint Powers Authority at (951) 656-7000 for additional information.
- 4. The attached notice shall be provided to all prospective purchasers of the proposed parcels and tenants or lessees of the buildings.
- 5. The proposed detention basins on the site (including water quality management basins) shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
- 6. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
- 7. This project has been evaluated for an industrial development consisting of 480,000 square feet of high cube warehouse area and 20,000 square feet of office area, and a commercial development consisting of 140,750 square feet of office area, a 3,062 square foot gas station convenience store and 12 fuel pumps, 9,015 square feet of restaurant dining area, 930 square feet of restaurant commercial kitchen, and 1,260 square feet of retail area. Any increase in building area, change in use (other than an increase in retail area in the Shops 1 building with a corresponding reduction in restaurant dining area therein) or modification of the tentative parcel map lot lines and areas will require an amended review by the Airport Land Use Commission.
- 8. The maximum square footage of restaurant dining area in Shops 1 building shall not exceed 6,015 square feet.
- 9. The maximum square footage of dining area in the fast food restaurant Pad A shall not exceed 3,000 square feet.
- 10. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission and March Air Reserve Base.
- 11. The Federal Aviation Administration has conducted an aeronautical study of the proposed project (Aeronautical Study Nos. 2018-AWP-9379-OE through 9391-OE) and has determined

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that neither marking nor lighting of the structure(s) is necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 L Change 1 and shall be maintained in accordance therewith for the life of the project.

12. The proposed buildings shall not exceed a height of 51 feet above ground level and a maximum elevation at top point of 1,777 feet above mean sea level.
13. The maximum height and top point elevation specified above shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in structure height or elevation shall not require further review by the Airport Land Use Commission.
14. Temporary construction equipment used during actual construction of the structure(s) shall not exceed 51 feet in height and a maximum elevation of 1,777 feet above mean sea level, unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.
15. Within five (5) days after construction of any individual building reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned or a decision is made not to construct the applicable structures(s).

V. MEETING SUMMARY

The following staff presented the subject proposal:

Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

No one spoke in favor, neutral or opposition to the project.

VI. ALUC COMMISSION ACTION

The ALUC by a unanimous vote of 4-0 found the project **CONSISTENT**. Absent: Commissioners Manos, Betts, Lyon

VII. CD

The entire discussion of this agenda item can be found on CD and referenced by the meeting time listed below. For a copy of the CD, please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at basantos@rivco.org.

ITEM 3.3: TIME: 9:40 A.M.

**AIRPORT LAND USE COMMISSION
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I. **AGENDA ITEM 3.4:** ZAP1026CH18 – The Merge Company I, LLC (Representative: Raymond A. Polverini) – City of Eastvale Case Nos. PLN18-20026 (General Plan Amendment, Change of Zone, Major Development Review, Tentative Parcel Map, Conditional Use Permit), a proposal to develop a commercial/industrial center consisting of 67,822 square feet of commercial building area on 10.8 acres and 336,501 square feet of industrial building area on 15.4 acres of a 26.2 gross acre site located on the northeast corner of Archibald Avenue and Limonite Avenue. The applicant also proposes to amend the commercial site’s General Plan land use designation from Light Industrial (LI) to Commercial Retail (CR) and change its zoning from Heavy Agricultural (A-2) to General Commercial (C-1/C-P), and to change the zoning of the industrial site from Heavy Agricultural (A-2) to Industrial Park (I-P). Also proposed is a tentative parcel map to subdivide the overall 26.2 gross acres into 17 parcels. (Airport Compatibility Zone C and D of the Chino Airport Influence Area).

II. **MAJOR ISSUES**

The project exceeds the Zone C single acre criterion of 150 people for the area including the Shops 1 building and a portion of the Major 1 Grocery building (169 people result). However, the applicant is proposing to incorporate risk reduction measures into the design of buildings. Specifically, both buildings will be single-story with no skylights. Additionally, the grocery store will be constructed with concrete block, windows will be limited to the southern face of the building, and the emergency exits will exceed requirements. These measures warrant a 15% bonus to the single acre criterion, resulting in an upgraded allowance of 173 people, with which the project’s single acre intensity of 169 would be consistent.

III. **STAFF RECOMMENDATION**

Staff recommends that the Commission find the proposed General Plan Amendment and Change of Zone CONSISTENT with the 2008 Chino Airport Land Use Compatibility Plan, and find the proposed Major Development Review, Tentative Parcel Map, and Conditional Use Permit CONSISTENT, subject to the conditions included herein.

IV. **PROJECT DESCRIPTION**

A proposal to develop a commercial/industrial center consisting of 67,822 square feet of commercial building area on 10.8 acres and 336,501 square feet of industrial building area on 15.4 acres of a 26.2 gross acre site. The applicant also proposes to amend the commercial site’s General Plan land use designation from Light Industrial (LI) to Commercial Retail (CR) and change its zoning from Heavy Agricultural (A-2) to General Commercial (C-1/C-P), and to change the zoning of the industrial site from Heavy Agricultural (A-2) to Industrial Park (I-P). Also proposed is a tentative parcel map to subdivide the overall 26.2 gross acres into 17 parcels.

CONDITIONS:

1. Any outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky.
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.

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- (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area, including landscaping utilizing water features, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, and incinerators.
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, hospitals, nursing homes (skilled nursing facilities), day care centers (including children's nurseries), and libraries.
 - (f) Highly noise-sensitive outdoor nonresidential uses and hazards to flight.
3. The attached notice shall be provided to all potential purchasers of the property and all potential tenants of the buildings, and shall be recorded as a deed notice.
 4. The proposed on-site detention basin shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basin that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
 5. This project has been evaluated as consisting of: (1) an industrial development consisting of 198,185 square feet of warehouse area, 80,950 square feet of manufacturing area, 27,366 square feet of mezzanine warehouse area, and 20,000 square feet of associated office area; and (2) a commercial development consisting of 2,955 square foot gas station convenience store and 8 fueling stations, 11,700 square foot drug store, 8,800 square foot retail store, 30,150 square foot grocery store, 4,000 square foot carwash facility, 2,317 square foot restaurant pad with 13 vehicle stack drive-thru, 3,900 square foot retail/restaurant pad, and 4,000 square foot retail/restaurant pad with 12 vehicle stack drive-thru. Any increase in building area, change in use or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria.
 6. The maximum square footage of restaurant dining area in Pad 1 building shall not exceed 1,117 square feet.
 7. The maximum square footage of restaurant dining area in Shops 2 shall not exceed 500 square feet.

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8. The maximum square footage of restaurant dining area in Pad 3 shall not exceed 1,010 square feet and shall be located in the easterly portion of the building (Zone D).
9. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission.
10. At least 3.85 acres of ALUC-eligible open areas (at least 75 feet in width and 300 feet in length), as depicted on the Open Space exhibit, a copy of which is attached, shall be kept obstacle and obstruction free per ALUC open area definition (no objects greater than four feet in height with a diameter of four inches or greater).
11. The design of Major 1 Grocery store building shall incorporate the risk-reduction design measures submitted with this project (see exhibit) which includes: building material shall be constructed out of concrete block, windows are limited to the southern face of the building, no building skylights, single story building only, and the addition of emergency exits above requirements.
12. The “Shops 1” building shall be single-story with no skylights.

V. MEETING SUMMARY

The following staff presented the subject proposal:

Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

No one spoke in favor, neutral or opposition to the project.

VI. ALUC COMMISSION ACTION

The ALUC by a unanimous vote of 4-0 found the project **CONSISTENT**. Absent: Commissioners Manos, Betts, Lyon

VII. CD

The entire discussion of this agenda item can be found on CD and referenced by the meeting time listed below. For a copy of the CD, please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at basantos@rivco.org.

ITEM 3.4 TIME: 9:51 A.M.

**AIRPORT LAND USE COMMISSION
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I. **AGENDA ITEM 3.5:** ZAP1075BD18 – City of Indio (Leila Namvar, Development Services Department). A proposal by the City of Indio to adopt a new General Plan 2040 to guide the future development of the City that focuses on revitalizing and connecting neighborhoods, establishing a human-scale network of complete streets and community open spaces, and enhancing community health and wellness. The General Plan includes the following elements/chapters: Vision and General Plan Strategies, Land Use and Urban Design, Mobility, Economic Development, Health and Equity, Parks, Recreation, and Open Space, Conservation, Infrastructure and Public Facilities, Safety, Noise, and Implementation. (The Housing Element is also part of the General Plan, but is not proposed for change as part of this effort.) The City includes land within all Compatibility Zones of the Bermuda Dunes Airport Influence Area.

II. **MAJOR ISSUES**

The Bermuda Dunes Airport Influence Area (AIA) extends into the City of Indio. The City includes land within all of the Compatibility Zones (A, B1, B2, C, D, and E), as well as areas outside the AIA. (Areas outside the AIA are not within ALUC's jurisdiction.) To the extent that the designations reflect existing land uses (including projects that have already received their final discretionary approval from the City of Indio), there is no conflict, as ALUC has no jurisdiction over existing land use. The proposed General Plan Land Use Map designates lands within Airport Compatibility Zones B1, B2, C, and D for land use densities and intensities that are not consistent with the 2004 Bermuda Dunes Airport Land Use Compatibility Plan. The proposed General Plan text will require additions and revisions in order to enable a consistency determination. As of the date of preparation of this staff report (August 23, 2018), staff review is ongoing. While we hope to be able to ultimately reach a finding of consistency with the 2004 Bermuda Dunes Airport Land Use Compatibility Plan, subject to certain changes being made to the maps and/or text of the proposed General Plan, at this time, we must recommend a continuance.

III. **STAFF RECOMMENDATION**

As initially submitted, the proposed General Plan Update is inconsistent with the Bermuda Dunes Airport Land Use Compatibility Plan. However, staff would prefer to find a path to consistency. At this time, staff recommends that the Commission open the public hearing, consider testimony, and CONTINUE consideration of this matter to its October 11, 2018 public hearing agenda.

As of the date of this staff report, the City of Indio has not requested or consented to a continuance. Due to the provisions of the Public Utilities Code, the Commission must render its determination within 60 days of project submittal unless the City agrees to a continuance. In the event that the City is not willing to agree to a continuance, staff would have to recommend a finding of inconsistency.

This recommendation is subject to change if ALUC staff is able to use the additional time between the date of this staff report and the hearing date to work out additional details with City staff.

IV. **PROJECT DESCRIPTION**

The City of Indio proposes to adopt General Plan 2040, a comprehensive update to the City's General Plan to guide the long-term development of the City and its sphere of influence. The General Plan Update includes the following elements: Land Use and Urban Design, Mobility, Economic Development, Public Health and Equity, Parks and Recreation, Conservation, Community Facilities and Infrastructure, Safety, Noise, and Implementation. (The Housing Element is also a part of the General Plan, but no changes are proposed through this effort.) The City includes land within all Compatibility Zones (A, B1, B2, C, D, and E) of the Bermuda Dunes Airport Influence Area.

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V. MEETING SUMMARY

The following staff presented the subject proposal:

Staff Planner: John Guerin at (951) 955-0982, or e-mail at jguerin@rivco.org

No one spoke in favor, neutral or opposition to the project.

VI. ALUC COMMISSION ACTION

Continued for discussion and public comment to October 11, 2018 due to lack of quorum. Recuse: Youmans; Absent: Commissioners Manos, Betts, Lyon

VII. CD

The entire discussion of this agenda item can be found on CD and referenced by the meeting time listed below. For a copy of the CD, please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at basantos@rivco.org.

ITEM 3.5: TIME: 10:00 A.M.

**AIRPORT LAND USE COMMISSION
MINUTE ORDER SEPTEMBER 13, 2018
RIVERSIDE MEETING**

I. **4.0 ADMINISTRATIVE ITEMS**

4.1 Director's Approvals – Information Only

4.2 ALUC Director's Report: The Path Forward Following the Release of the 2018 Air Installation Compatible Use Zones Report for March Air Reserve Base/Inland Port Airport

Simon Housman, ALUC Director presented a bar chart to the Commission regarding the future timeline estimates on the March Airport Land Use Compatibility Plan (ALUCP) and Joint Land Use Study process (JLUS).

II. **5.0 APPROVAL OF MINUTES**

The ALUC by a unanimous vote of 4-0 approved the August 9, 2018 minutes. Absent: Manos, Betts and Lyon

III. **6.0 ORAL COMMUNICATION ON ANY MATTER NOT ON THE AGENDA**

None

IV. **7.0 COMMISSIONER'S COMMENTS**

Richard Stewart, Acting Chairman advised that the Technical Advisory Committee (TAC) for the March Joint Powers Authority approved flights by Amazon into the March Global Port which is good due to its valuable usage for fees of that runway. He also noticed the significant amount of building development around the airports and commented the very important duties ALUC staff and the Commissioners have regarding the future of all airports.

V. **8.0 ADJOURNMENT**

Commissioner Steven Stewart adjourned the meeting at 10:23 a.m.

VI. **CD**

The entire discussion of this agenda item can be found on CD and referenced by the meeting time listed below. For a copy of the CD, please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at basantos@rivco.org.

ITEM 4.0: TIME IS: 10:12 A.M.