

AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY AGENDA

Eastern Municipal Water District - Board Chambers
2270 Trumble Road
Perris, California

Thursday 9:00 A.M., December 8, 2016

NOTE: If you wish to speak, please complete a "SPEAKER IDENTIFICATION FORM" and give it to the Secretary. The purpose of the public hearing is to allow interested parties to express their concerns. Comments shall be limited to 5 minutes and to matters relevant to the item under consideration. Please do not repeat information already given. If you have no additional information, but wish to be on record, simply give your name and address and state that you agree with the previous speaker(s). Also please be aware that the indicated staff recommendation shown below may differ from that presented to the Commission during the public hearing.

Non-exempt materials related to an item on this agenda submitted to the Airport Land Use Commission or its staff after distribution of the agenda packet are available for public inspection in the Airport Land Use Commission's office located at 4080 Lemon Street, 14th Floor, Riverside, CA 92501 during normal business hours.

In compliance with the Americans with Disabilities Act, if any accommodations are needed, please contact Barbara Santos at (951) 955-5132 or E-mail at basantos@rctlma.org. Request should be made at least 48 hours or as soon as possible prior to the scheduled meeting.

1.0 INTRODUCTIONS

1.1 CALL TO ORDER

1.2 SALUTE TO FLAG

1.3 ROLL CALL

2.0 PUBLIC HEARING: CONTINUED ITEMS

MARCH AIR RESERVE BASE

- 2.1 ZAP1206MA16 – Optimus Building Corporation (Representative: Gary Hamro) – City of Perris Case No. DPR 14-01-0015 (Development Plan Review). The applicant is proposing a revision to plans for a two-building warehousing project previously determined to be consistent pursuant to ALUC Case No. ZAP1102MA14, specifically to increase the office area in Building A (which is not located in an Accident Potential Zone) from 15,000 square feet to 45,000 square feet. As amended, Building A would provide 45,000 square feet of office area and 867,338 square feet of warehouse area. No changes are proposed for Building B. The overall floor areas of Buildings A (912,338 square feet) and B (125,437 square feet) would remain the same as originally proposed. The 53.56-acre (gross) project site is located easterly of Patterson Avenue, northerly of Markham Street, westerly of Webster Avenue, and southerly of Nance Street (Airport Compatibility Zones B1-APZ I, B2, and C1 of the March Air Reserve Base/Inland Port Airport Influence Area). Continued from September 8, August 11, October 13, 2016 and November 10, 2016. ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rctlma.org

Staff Recommendation: CASE WITHDRAWN



CHAIR
Simon Housman
Rancho Mirage

VICE CHAIRMAN
Rod Ballance
Riverside

COMMISSIONERS
Arthur Butler
Riverside

John Lyon
Riverside

Glen Holmes
Hemet

Steve Manos
Lake Elsinore

Russell Betts
Desert Hot Springs

STAFF

Director
Ed Cooper

John Guerin
Paul Rull
Barbara Santos

County Administrative Center
4080 Lemon St, 14th Floor
Riverside, CA 92501
(951) 955-5132

www.rcaluc.org

MARCH AIR RESERVE BASE

- 2.2 ZAP1204MA16 – Duke Realty, Adam Schmid (Representative: Albert A. Webb Associates, Nicole Torstvet) – City of Perris Case No. PLN 16-00008 (Development Plan Review). The applicant is proposing to develop a 668,381 square foot warehouse/distribution center on 30.7 acres. The building floor plan consists of 649,481 square feet of warehouse area and 19,200 square feet of office area. The project site is located southerly of Markham Street, westerly of Indian Avenue, easterly of Barrett Avenue and northerly of Perry Street. (Airport Compatibility Zones B1-APZ I, B2, and C1 of the March Air Reserve Base/Inland Port Airport Influence Area). Continued from September 8, August 11, October 13, 2016 and November 10, 2016. ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rctlma.org

Staff Recommendation: CASE WITHDRAWN

3.0 PUBLIC HEARING: NEW ITEMS**JACQUELINE COCHRAN REGIONAL AIRPORT**

- 3.1 ZAP1036TH16 – Thermal Operating Company, LLC (Representative: Fayres Hall, Albert A. Webb and Associates) – County of Riverside Planning Case No. PP26120 (Plot Plan). The applicant is proposing to develop eight industrial (predominately warehouse) 2-story buildings ranging from 13,600 to 20,300 square feet in area, with a cumulative total gross floor area of 135,549 square feet on 3.92 acres (Lots 1 through 8 of Tentative Parcel Map No. 36735) located easterly of Tyler Street, along the south side of Jasper Lane (a private road), and northerly of 62nd Avenue. (Airport Compatibility Zones C and D of the Jacqueline Cochran Regional Airport Influence Area). ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rctlma.org

Staff Recommendation: CONSISTENT

- 3.2 ZAP1037TH16 – Thermal Operating Company, LLC (Representative: Fayres Hall, Albert A. Webb and Associates) – County of Riverside Planning Case No. PP26121 (Plot Plan). The applicant is proposing to develop 14 industrial (predominately warehouse) 2-story buildings ranging from 19,400 to 64,500 square feet in area, with a cumulative total gross floor area of 361,800 square feet on 12.8 acres (Lots 1 through 14 of Parcel Map No. 36844) and an overflow 80-space parking lot on Lot 15, all located easterly of Tyler Street and northerly of 62nd Avenue, along the west side of Ascot Drive (a private road), and southerly of Jasper Lane. (Airport Compatibility Zone C of the Jacqueline Cochran Regional Airport Influence Area). ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rctlma.org

Staff Recommendation: CONSISTENT

BANNING MUNICIPAL AIRPORT

- 3.3 ZAP1020BA15 – RMG Residential 2010, LLLP (Representative: United Engineering Group) – City of Banning Planning Case No. 15-70004 (Tentative Tract Map No. 36710). The applicant is proposing to divide 10.67 acres (Assessor's Parcel Numbers 534-183-014, 534-200-004, 534-200-008, and 534-200-047) into 46 single-family residential lots, one drainage basin lot, and one open space lot. The project site is located on the northerly side of Wilson Street, westerly of Florida Street, southerly of Hoffer Street and easterly of Alessandro Road in the City of Banning (Airport Compatibility Zone D of the Banning Municipal Airport Influence Area). ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rctlma.org

Staff Recommendation: INCONSISTENT

BERMUDA DUNES EXECUTIVE AIRPORT

- 3.4 ZAP1066BD16 – Fiesta Ford, Inc. (Representative: KSC, Inc.) – City of Indio Planning Case Nos. 16-10-403 (Design Review) and 16-10-1018 (Conditional Use Permit). The applicant is proposing to develop a Fiesta Ford Quick Lane and Collision Center auto repair facility consisting of a 10,314 square foot service repair building and 17,826 square foot collision building on 2.8 acres (Assessor's Parcel Numbers 607-230-038 and 607-230-040). The project site is located on the southeast corner of Avenue 40 and Adams Street in the City of Indio (Airport Compatibility Zone B1, C and D of the Bermuda Dunes Airport Influence Area). ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rctlma.org

Staff Recommendation: CONDITIONALLY CONSISTENT

- 3.5 ZAP1067BD16 – Jefferson Development 16, LLC (Representative: William Sharon, McGee Sharon Architects, Inc.) – City of Indio Planning Case Nos. CUP16-10-1019 (Conditional Use Permit) and DR16-10-404 (Design Review). A proposal to develop a commercial center with retail and restaurant buildings, convenience store with fuel pumps, and self-storage facility on 19.26 acres located along the northerly side of Indio Boulevard, easterly of its overcrossing of Interstate 10, southerly of the rail line and freeway, and westerly of Madio and Madison Streets. The project includes 50,300 square feet in five retail buildings (including a 16,200 square foot building suitable for a chain drug store pharmacy), five freestanding pads along the frontage (four with drive-thru lanes) with a cumulative floor area of 21,000 square feet, a 4,300 square foot convenience store with deli, ten gasoline pumps (20 service locations), and automated drive-thru car wash, and a seven-building 81,915 square foot self-storage facility with on-site caretaker's residence (Airport Compatibility Zones A and B1 of the Bermuda Dunes Airport Influence Area). ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rctlma.org

Staff Recommendation: INCONSISTENT

MARCH AIR RESERVE BASE

- 3.6 ZAP1220MA16 – Rev Wheel LLC (Representative: SDH & Associates, Inc.) – City of Riverside Planning Case Nos. P15-1035, P16-0556 and P16-0557. A proposal to develop eight industrial buildings (predominately warehouse) ranging from 12,015 to 35,661 square feet in gross floor area, with a cumulative total of 179,710 square feet on 10.4 acres located westerly of Old 215 Frontage Road, northerly of Alessandro Boulevard, southerly of Cottonwood Avenue, and easterly of Interstate 215, and to divide 6.5 acres of the site along the westerly side of Old 215 Frontage Road into six lots so that buildings 1-6 will each be located on a separate lot. The applicant is also proposing to amend the Sycamore Canyon Business Park Specific Plan land use designation from Retail Business Office and Industrial Support to Industrial, and to incorporate development standards for these lots in the Specific Plan. (Airport Compatibility Zone B1-APZ II of the March Air Reserve Base/Inland Port Airport Influence Area). ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rctlma.org

Staff Recommendation: CONSISTENT (SPA); INCONSISTENT (Parcel Map and Plot Plan)

4.0 PUBLIC HEARING 9:30 A.M.: HEMET-RYAN AIRPORT LAND USE COMPATIBILITY PLAN

- 4.1 ZAPEA02HR13 – Hemet-Ryan Airport Land Use Compatibility Plan (ALUCP) – SCH#2016111015. The Riverside County Airport Land Use Commission (ALUC) proposes to adopt an ALUCP establishing criteria for the evaluation of the compatibility of land uses proposed to be located in the vicinity of Hemet-Ryan Airport, in accordance with ALUC's duties as set forth in State law. The proposed ALUCP includes an expansion of the airport influence area within portions of the City of Hemet and nearby unincorporated Riverside County areas. Compatibility criteria in the proposed ALUCP address four types of compatibility concerns: safety, noise, airspace protection, and overflight. The provisions of the ALUCP apply only to future development and not existing land uses. The ALUCP does not propose the development or acquisition of land, nor does the ALUC have any jurisdiction over the operation of the airport or aircraft in flight. ALUC Staff Planner: John Guerin at (951) 955-0982, or e-mail at jguerin@rctlma.org

Staff Recommendation: ADOPT NEGATIVE DECLARATION AND COMPATIBILITY PLAN

5.0 ADMINISTRATIVE ITEMS

5.1 Director's Approvals

5.2 Specific Delegation of Authority: Contractor's Storage Yard in March Zone C2

6.0 APPROVAL OF MINUTES

November 10, 2016

7.0 ORAL COMMUNICATION ON ANY MATTER NOT ON THE AGENDA**8.0 COMMISSIONER'S COMMENTS**

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 2.1 3.2

HEARING DATE: December 8, November 10, October 13, September 8, 2016
(continued from November 10, October 13, September 8
and August 11, 2016)

CASE NUMBER: ZAP1206MA16 – Optimus Building Corporation
(Representative: Gary Hamro)

APPROVING JURISDICTION: City of Perris

JURISDICTION CASE NO: DPR 14-01-0015 (Development Plan Review)

ALUC CASE STATUS: The applicant has withdrawn this case from further ALUC consideration due to the City having adopted the General Plan Amendment, Specific Plan Amendment, and zoning ordinance amendment needed to achieve consistency and to be able to evaluate compatibility of non-legislative actions within the portion of the March Airport Influence Area within its jurisdiction. The underlying project will continue to be considered by the City of Perris.

Y:\AIRPORT CASE FILES\March\ZAP1206MA16\ZAP1206MA16sdecsr withdrawal.doc

Gary Hamro

12040 East Florence Ave

Santa Fe Springs, CA 90670

Date 11/16/16

RE: Request to Withdraw ALUC application ZAP1206MA16

As the authorized applicant for this project, I request to withdraw my ALUC application ZAP1206MA16 from the ALUC process effective immediately.

Sincerely

Handwritten signature of Gary Hamro, consisting of a stylized 'G' and 'H' followed by a long horizontal line, and the date '11-16-16' written to the right.

Gary Hamro

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 2.2 3.3

HEARING DATE: December 8 ~~November 10, October 13, September 8, 2016~~
(continued from ~~November 10, October 13, September 8~~
and August 11, 2016)

CASE NUMBER: ZAP1204MA16 – Duke Realty, Adam Schmid
(Representative: Albert A. Webb Associates, Nicole
Torstvet)

APPROVING JURISDICTION: City of Perris

JURISDICTION CASE NO: PLN 16-00008 (Development Plan Review)

ALUC CASE STATUS: *The applicant has withdrawn this case from further ALUC consideration due to the City having adopted the General Plan Amendment, Specific Plan Amendment, and zoning ordinance amendment needed to achieve consistency and to be able to evaluate compatibility of non-legislative actions within the portion of the March Airport Influence Area within its jurisdiction. The underlying project will continue to be considered by the City of Perris.*

Y:\AIRPORT CASE FILES\March\ZAP1204MA16\ZAP1204MA16decsr.doc



November 17, 2016

Corporate Headquarters

3788 McCray Street
Riverside, CA 92506
951.686.1070

Palm Desert Office

41-990 Cook St., Bldg. I - #801B
Palm Desert, CA 92211
951.686.1070

Murrieta Office

41391 Kalmia Street #320
Murrieta, CA 92562
951.686.1070

Mr. Paul Rull
Airport Land Use Commission
4080 Lemon Street, 14th Floor
Riverside, CA 92501

RE: ZAP1204 MA16 Duke Perris TPM 37187

Dear Paul,

On behalf of Duke Realty, we hereby request that you withdraw the application for case # ZAP1204 MA16 for the Duke Perris TPM # 37187. If you have any questions please contact Sandy Chandler at 951-686-1070.

Sincerely,

ALBERT A. WEBB ASSOCIATES

A handwritten signature in blue ink, appearing to read "Sandra G. Chandler".

Sandra G. Chandler, AICP
Entitlement Manager



**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.1

HEARING DATE: December 8, 2016

CASE NUMBER: ZAP1036TH16 – Thermal Operating Company, LLC
(Representative: Fayres Hall, Albert A. Webb and Associates)

APPROVING JURISDICTION: County of Riverside

JURISDICTION CASE NO: PP26120 (Plot Plan)

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends a finding of CONSISTENCY for the Plot Plan, subject to the conditions included herein.

PROJECT DESCRIPTION: The applicant proposes to develop eight industrial (predominately warehouse) 2-story buildings ranging from 13,600 to 20,300 square feet in area, with a cumulative total gross floor area of 135,549 square feet on 3.92 net acres (4.69 gross).

The Commission had previously found the original proposal for a tentative parcel map to divide 46.22 acres into nine (9) numbered lots, plus four letter lots for a private street (Jasper Lane), detention basin, and landscaped roadway frontage, consistent via ZAP1030TH15.

PROJECT LOCATION: The project site is located easterly of Tyler Street, along the south side of Jasper Lane (a private road), and northerly of 62nd Avenue in the unincorporated community of Thermal, approximately 5,315 feet southwesterly of the southerly terminus of Runway 17-35 at Jacqueline Cochran Regional Airport.

LAND USE PLAN: 2005 Jacqueline Cochran Regional Airport Land Use Compatibility Plan (last amended in 2006)

- a. Airport Influence Area: Jacqueline Cochran Regional Airport
- b. Land Use Policy: Airport Compatibility Zones C and D
- c. Noise Levels: Below 55 CNEL from aircraft at this location

BACKGROUND:

Non-Residential Average Intensity: Compatibility Zone C limits average intensity to 75 people per acre and Compatibility Zone D limits average intensity to 100 people per acre. Approximately 3.72 gross acres of the site are located within Compatibility Zone C, with the remaining 0.96 gross acre area located within Compatibility Zone D.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, the following rates were used to calculate the occupancy for the proposed buildings:

- Warehouse area – 1 person per 500 square feet
- Office area – 1 person per 200 square feet

Based on the site plan provided, the cumulative total building area of all eight lots is 135,549 square feet (115,217 square feet of warehouse area and 20,352 square feet of office), which would accommodate a total of 332 people on 4.69 acres, resulting in an average intensity of 71 people per acre.

The site is divided into Compatibility Zones C and D, with approximately 107,094 square feet of building area and occupancy of 262 people in Zone C, and 28,454 square feet of building area and occupancy of 69 people in Zone D. This results in an average intensity of 70 people per acre in Compatibility Zone C, which is consistent with the Zone C average intensity criterion of 75 people per acre, and an average intensity of 66 people per acre in Zone D, which is consistent with the Zone D average intensity criterion of 100 people per acre.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle in the absence of more precise data). Based on the number of parking spaces required (159 spaces), the total occupancy would be estimated at 239 people for an average acre intensity of 51 people per acre over the entirety of the gross site. If these numbers were broken down by Compatibility Zone, there are 35 parking spaces required in Zone D (resulting in an occupancy of 53 people for an average intensity of 51 people per acre), and there are 124 parking spaces required in Zone C (resulting in an occupancy of 186 people for an average intensity of 50 people per acre), both of which are consistent with average intensity criteria.

Non-Residential Single-Acre Intensity: Compatibility Zone C limits single-acre intensity to 150 people and Compatibility Zone D limits single-acre intensity to 300 people. Based on the site plan and floor plans provided for the project, the most intense single-acre would occur on Lots 7 and 8 in Compatibility Zone C. This includes 32,512 square feet of warehouse area and 5,737 square feet of office area, resulting in a single-acre intensity of 94 people, which is consistent with the Compatibility Zone C criterion of 150 people.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Zone C (Children's schools, day care centers, libraries, hospitals, nursing homes, buildings with more than 2 aboveground habitable floors, highly noise-sensitive outdoor non-

residential uses, aboveground bulk storage of hazardous materials, critical community infrastructure facilities, and hazards to flight) or Zone D (highly noise-sensitive outdoor non-residential uses, and hazards to flight) within the project.

Noise: The property lies outside the areas subject to average exterior noise levels greater than 55 CNEL. Therefore, no special measures to mitigate aircraft-generated noise are required.

Part 77: The elevation of Runway 18-36 at its southerly terminus is approximately -134 feet, i.e., 134 feet below mean sea level (MSL). At a distance of approximately 5,315 feet from the runway, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding -81 feet MSL. The site elevation is approximately -145 feet MSL and the maximum height of the buildings is 35 feet for a top point elevation of -110 feet MSL. Therefore, review of buildings by the FAA Obstruction Evaluation Service (FAA OES) is not required.

Open Area: The site is less than ten acres in area; therefore, the project is not subject to Compatibility Zones C and D open area requirements.

CONDITIONS:

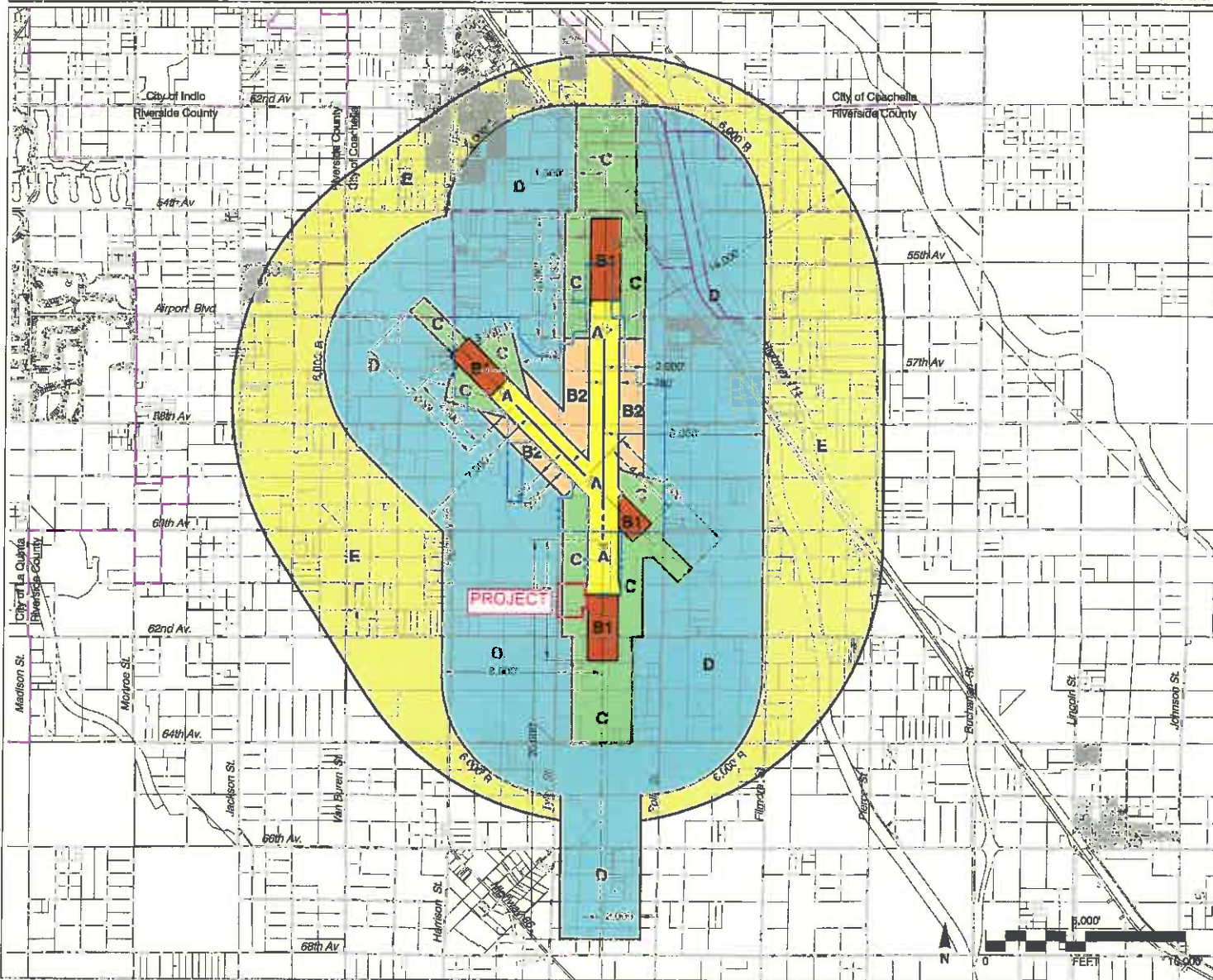
1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting plans, if any, shall be transmitted to Riverside County Economic Development Agency – Aviation Division personnel and to the Jacqueline Cochran Regional Airport for review and comment. (Failure to comment within thirty days shall be considered to constitute acceptability on the part of the airport manager.)
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations, or any type of strobe light, toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, livestock operations, production of cereal grains, sunflower, and row crops, artificial marshes, wastewater management facilities, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes,

construction and demolition debris facilities, fly ash disposal, and incinerators.)

- (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, day care centers, libraries, hospitals, nursing homes, places of worship, highly noise-sensitive outdoor non-residential uses, and hazards to flight.
3. The attached notice shall be provided to all prospective purchasers of the property and tenants of the buildings thereon, and shall be recorded as a deed notice.
 4. Any new detention basin(s) shall be designed so as to provide a maximum 48-hour detention period for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basin that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
 5. A finding of consistency for this project is based on the proposed office and warehouse use. Any other subsequent use will be subject to ALUC review.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Legend

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E

Boundary Lines

- Airport Property Line - Existing
- Airport Property Line - Planned
- City Limits

Note

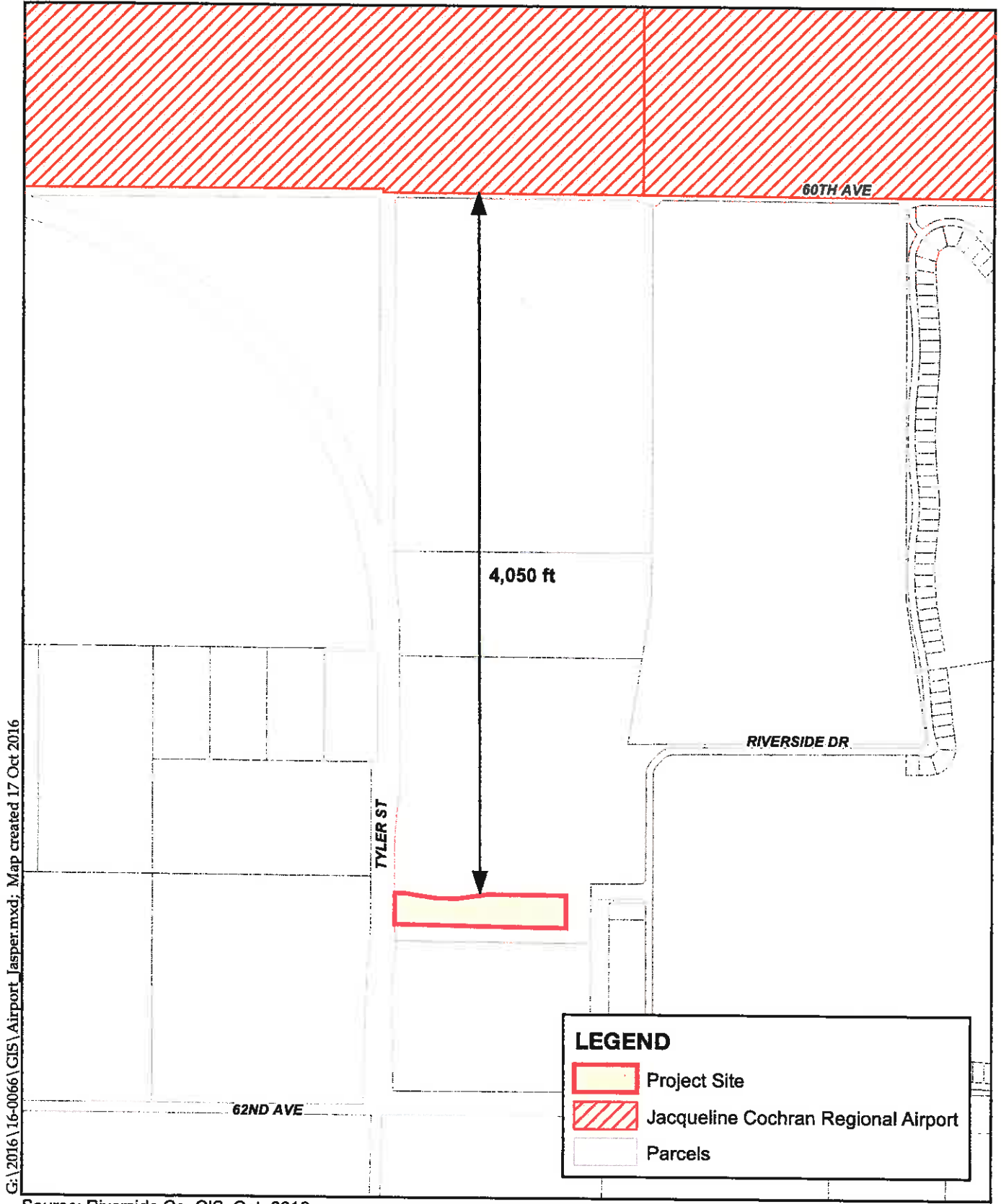
Except for southern extension, Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

See Chapter 2, Table 2A for compatibility criteria associated with this map.

Riverside County
 Airport Land Use Commission
Riverside County
Airport Land Use Compatibility Plan
Policy Document
 (Adopted June 2005)

Map JC-1

Compatibility Map
 Jacqueline Cochran Regional Airport



Source: Riverside Co. GIS, Oct. 2016.

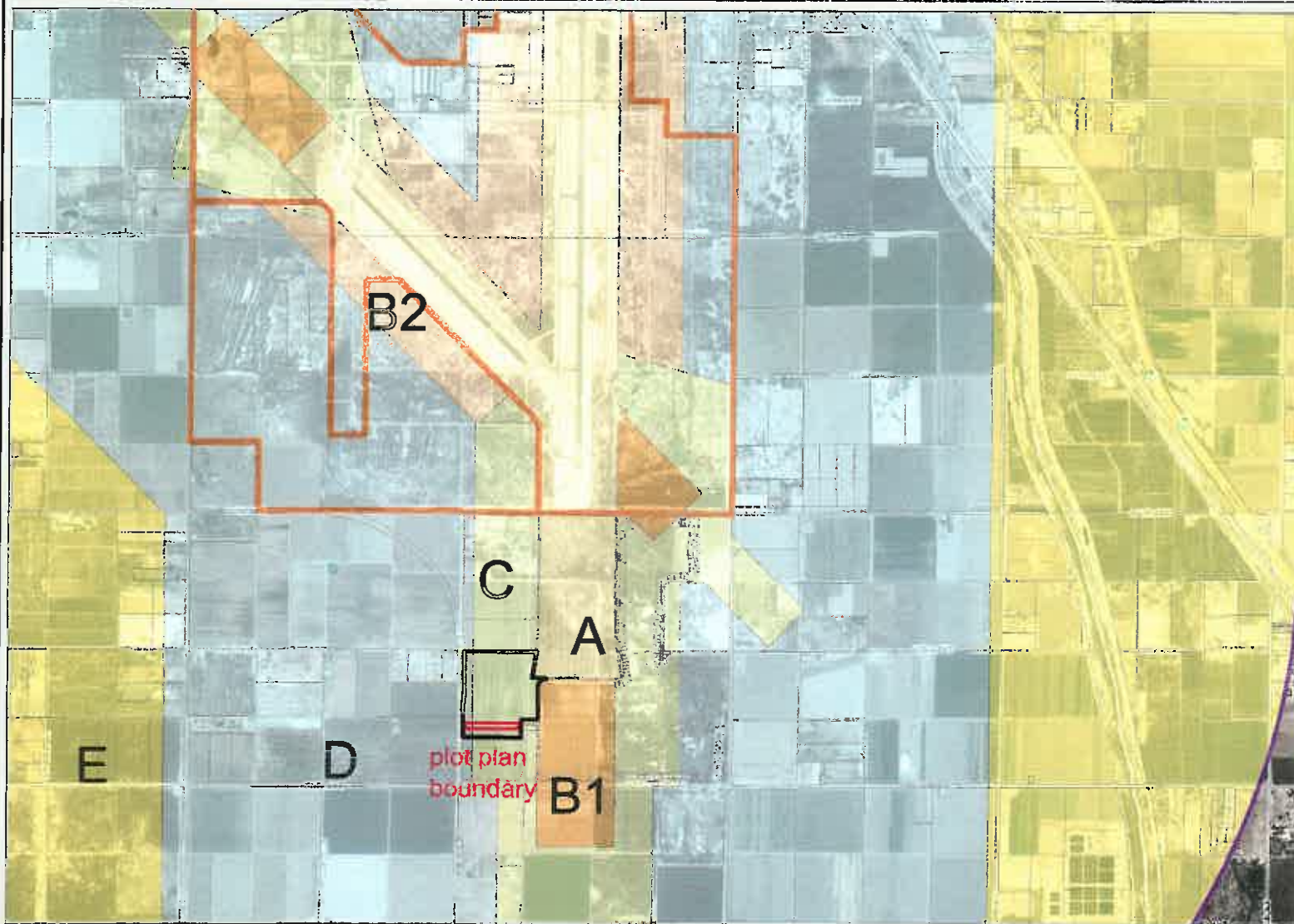
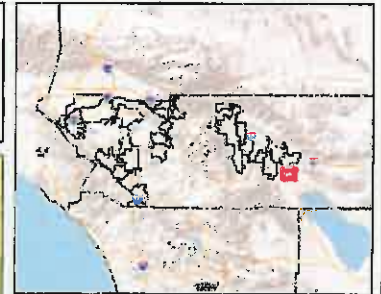
Relation to Nearby Airport
Jasper



0 800 1,600 Feet



My Map



Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

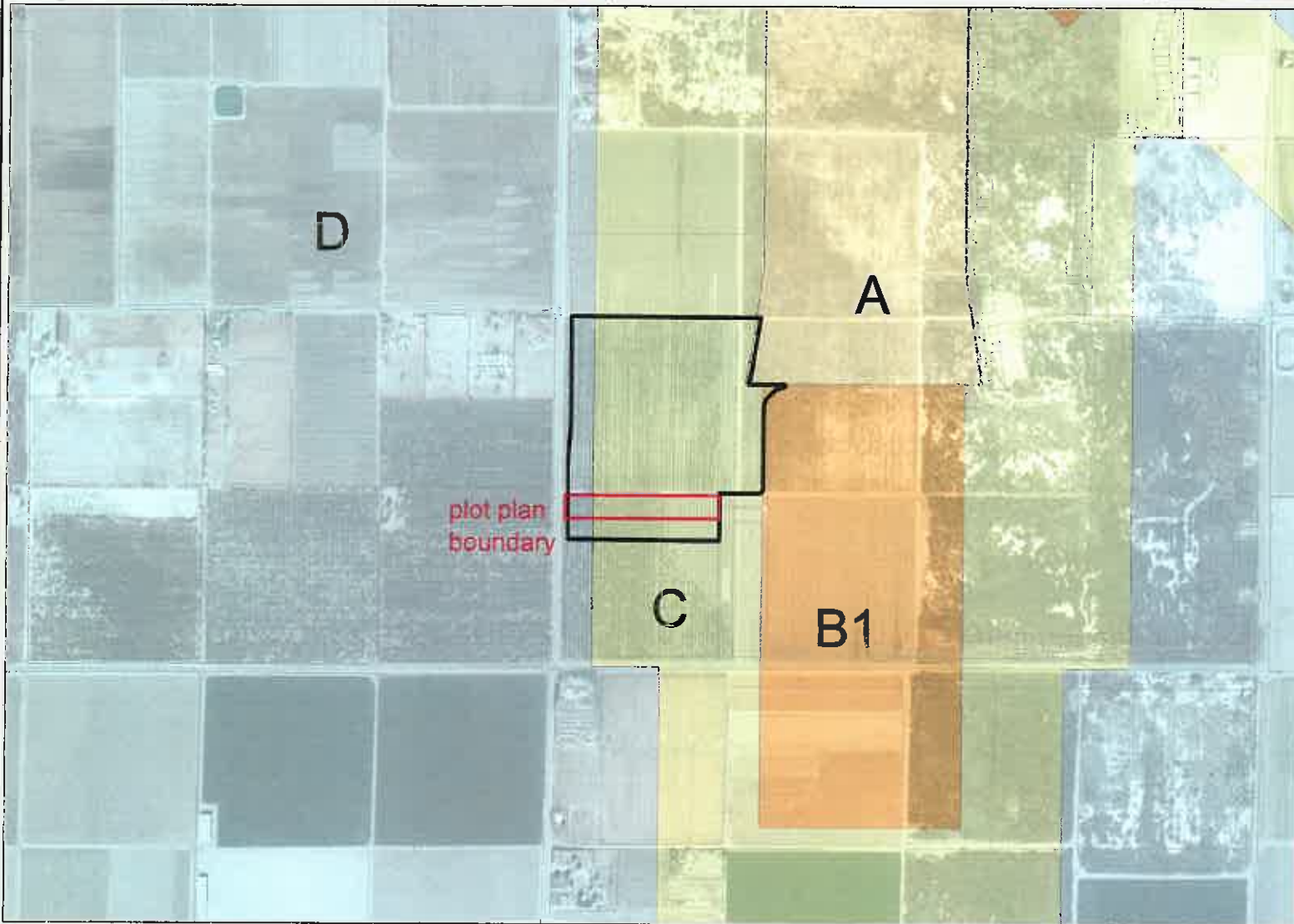
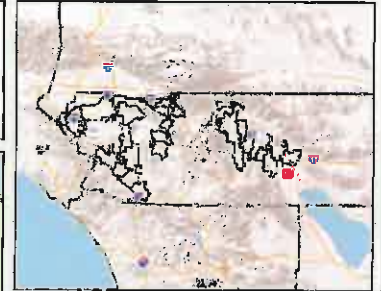


REPORT PRINTED ON... 11/7/2016 10:24:19 AM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



0 1,374 2,747 Feet



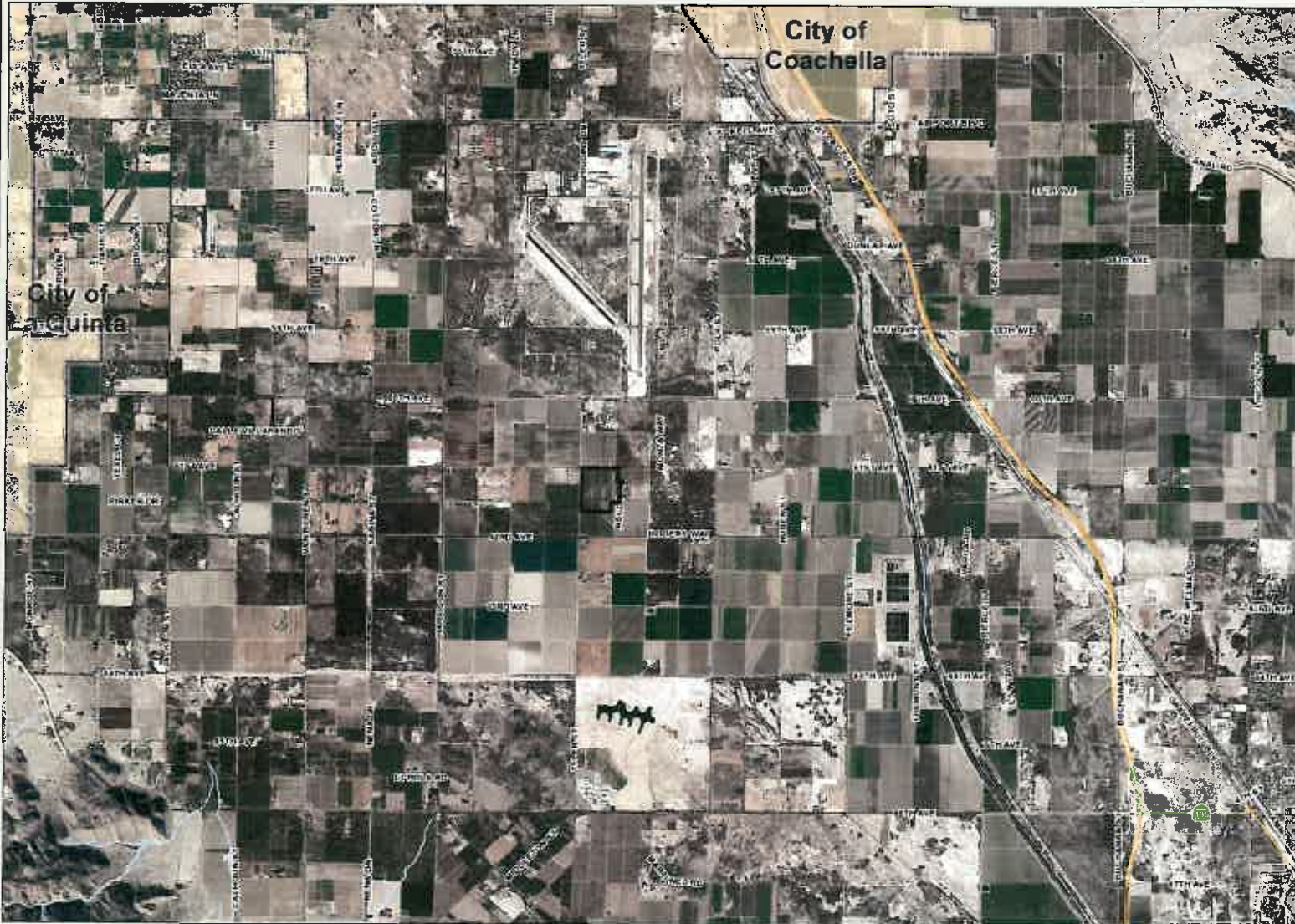
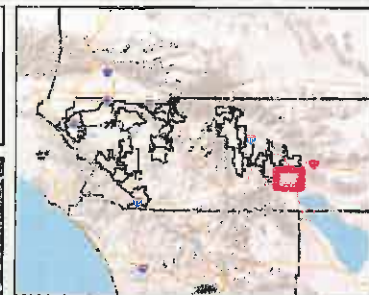
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/7/2016 10:22:44 AM

© Riverside County RCIT GIS

Notes

My Map



- Legend**
- City Boundaries
 - Cities**
 - highways_large**
 - HWY
 - INTERCHANGE
 - INTERSTATE
 - USHWY
 - majorroads
 - counties
 - cities



0 7,155 14,310 Feet



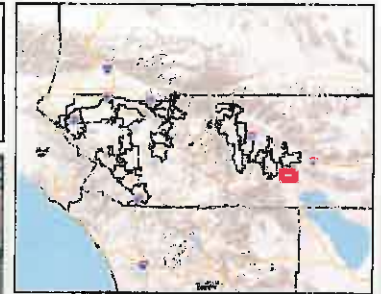
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/7/2016 10:26:18 AM

© Riverside County RCIT GIS

Notes

My Map



Legend

- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 3,578 7,155 Feet



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/7/2016 10:26:49 AM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities**
- roads**
- highways**
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrography**
- waterbodies**
- Lakes
- Rivers



0 1,789 3,578 Feet



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/7/2016 10:27:21 AM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities**
- roadsanno**
- highways**
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines**
- waterbodies**
- Lakes
- Rivers



0 1,374 2,747 Feet



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/7/2016 10:23:23 AM

© Riverside County RCIT GIS

Notes



SCHEMATIC ELEVATION IMAGERY - CONTEMPORARY

RITA

 Architecture

 and Planning, P.C.

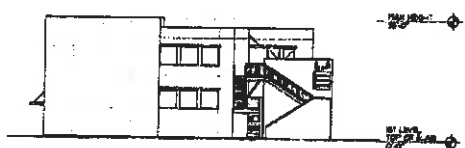
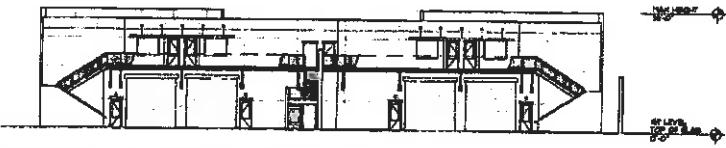
 4000 E. 17th Street, Suite 100

 Phoenix, AZ 85032

 Phone: (602) 998-1111

 Fax: (602) 998-1112

 Email: info@ritaz.com



SCHEMATIC ELEVATIONS - CONTEMPORARY



No.	Date	Revision

Project Number: **14-009**

THE THERMAL CLUB

JASPER LAKE

 THERMAL, CALIFORNIA

REVIEW

 By:

 Date: _____

SITE PLAN

 Project Number:

ALI

Lot	Net Area (Sq Ft)	Gross Area (Sq Ft)	Gross Area (Acres)	Avg Criteria	Max Occupancy
1	20,676	24,323	0.558	100	55.84
2	15,951	20,363	0.467	100	46.75
3	19,595	26,413	0.606	75	45.48
4	19,172	24,358	0.559	75	41.94
5	20,727	24,080	0.553	75	41.46
6	24,065	27,553	0.633	75	47.44
7	24,916	28,533	0.655	75	49.13
8	25,263	28,920	0.664	75	49.79

NOTICE OF PUBLIC HEARING RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Thursday from 8:00 a.m. to 5:00 p.m., except Thursday, November 24 (Thanksgiving Day) and by prescheduled appointment on Friday, December 2 from 8:30 a.m. to 5:00 p.m.

ATTENTION: ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. The County of Riverside may hold hearings on this project and should be contacted on non-ALUC issues.

PLACE OF HEARING: Eastern Municipal Water District
2270 Trumble Road
Perris, California

DATE OF HEARING: December 8, 2016

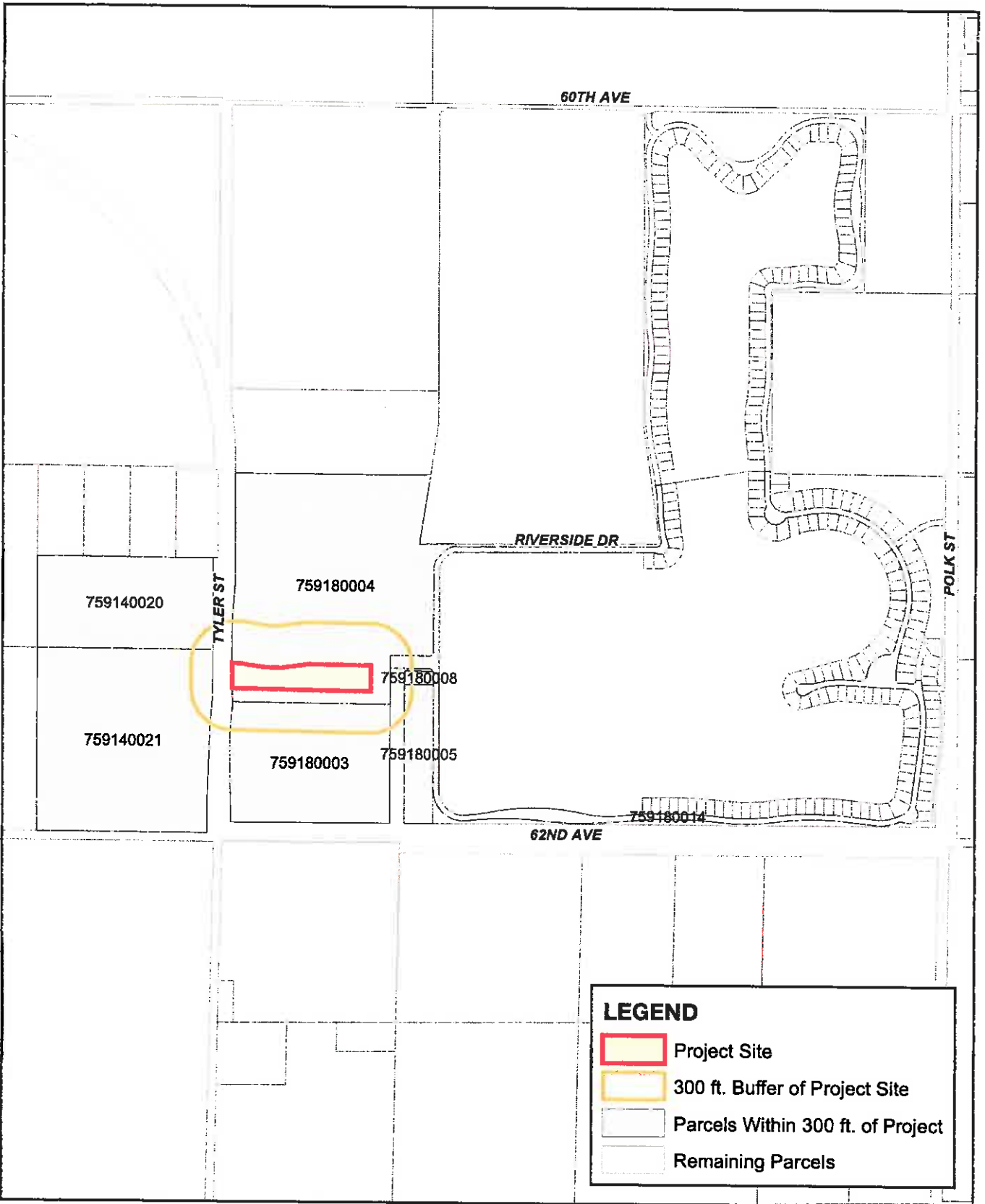
TIME OF HEARING: 9:00 A.M.

CASE DESCRIPTION:

ZAP1036TH16 – Thermal Operating Company, LLC (Representative: Fayres Hall, Albert A. Webb and Associates) – County of Riverside Planning Case No. PP26120 (Plot Plan). The applicant is proposing to develop eight industrial (predominately warehouse) 2-story buildings ranging from 13,600 to 20,300 square feet in area, with a cumulative total gross floor area of 135,549 square feet on 3.92 acres (Lots 1 through 8 of Tentative Parcel Map No. 36735) located easterly of Tyler Street, along the south side of Jasper Lane (a private road), and northerly of 62nd Avenue. (Airport Compatibility Zones C and D of the Jacqueline Cochran Regional Airport Influence Area)

FURTHER INFORMATION: Contact Paul Rull at (951) 955-6893. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Mr. Russell Brady of the County of Riverside Planning Department at (951) 955-3025.





C:\2016\16-0066\GIS\Parcels_300_Jasper.mxd; Map created 17 Oct 2016

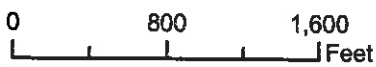


Source: Riverside Co. GIS, Oct. 2016.

Parcels Within 300 ft. of Project Jasper

LEGEND

-  Project Site
-  300 ft. Buffer of Project Site
-  Parcels Within 300 ft. of Project
-  Remaining Parcels



APPLICATION FOR MAJOR LAND USE ACTION REVIEW
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

ALUC Identification No.

ZAP1036 TH16

PROJECT PROPONENT (TO BE COMPLETED BY APPLICANT)

Date of Application _____

Property Owner JTM Land Company, LLC c/o Tim Rogers

Phone Number 310-453-0111

Mailing Address 1983 W. 190th Street, Suite 100
Torrance, CA 90504

Agent (if any) Albert A Webb Associates

Phone Number 951-686-1070

Mailing Address 3788 McCray Street
Riverside, CA 92506

JCRA
Zones
C+D

PROJECT LOCATION (TO BE COMPLETED BY APPLICANT)

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address North of Avenue 62, south of Avenue 60, east of Tyler Street and west of Polk Street

Assessor's Parcel No. Portion of APN 759-180-004

Parcel Size 3.92 within Plot Plan

Subdivision Name TPM36735

Lot Number Lots 1-8

Zoning Classification Kohl Ranch Specific Plan #30

PROJECT DESCRIPTION (TO BE COMPLETED BY APPLICANT)

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe) Vacant previously disturbed.

Proposed Land Use (describe) Plot Plan to include Lots 1-8 consisting of one building per lot.

For Residential Uses Number of Parcels or Units on Site (exclude secondary units) N/A

For Other Land Uses Hours of Use Accessible 24 hours/ 7 days a week.

(See Appendix C) Number of People on Site Maximum Number 377.83

Method of Calculation Maximum Occupancy

Height Data Height above Ground or Tallest Object (including antennas and trees) 35' Max ft.

Highest Elevation (above sea level) of Any Object or Terrain on Site 362.4 (-137.6) ft.

Flight Hazards Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight? Yes No

If yes, describe _____

REFERRING AGENCY (APPLICANT OR JURISDICTION TO COMPLETE)

Date Received	<u>10/12/2016</u>	Type of Project
Agency Name	<u>County of Riverside Planning Department</u>	<input type="checkbox"/> General Plan Amendment
Staff Contact	<u>Planner not yet assigned</u>	<input type="checkbox"/> Zoning Amendment or Variance
Phone Number	<u>951-955-3200</u>	<input type="checkbox"/> Subdivision Approval
Agency's Project No.	<u>PP26120</u>	<input type="checkbox"/> Use Permit
		<input type="checkbox"/> Public Facility
		<input checked="" type="checkbox"/> Other <u>Plot Plan</u>

A. NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. SUBMISSION PACKAGE:

ALUC REVIEW

- 1 Completed Application Form
- 1 Project Site Plan – Folded (8-1/2 x 14 max.)
- 1 Elevations of Buildings - Folded
- 1 Each . 8 ½ x 11 reduced copy of the above
- 1 8 ½ x 11 reduced copy showing project in relationship to airport.
- 1 Set . Floor plans for non-residential projects
- 4 Sets. . Gummed address labels of the Owner and representative (*See Proponent*).
- 1 Set. . Gummed address labels of all property owners within a 300' radius of the project site. If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10), with ALUC return address.
- 4 Sets. . Gummed address labels of the referring agency (City or County).
- 1 Check for Fee (See Item "C" below)

STAFF REVIEW (Consult with ALUC staff planner as to whether project qualifies)

- 1 Completed Application Form
- 1 Project Site Plans – Folded (8-1/2 x 14 max.)
- 1 Elevations of Buildings - Folded
- 1 8 ½ x 11 Vicinity Map
- 1 Set . Gummed address labels of the Owner and representative (*See Proponent*).
- 1 Set . Gummed address labels of the referring agency.
- 1 Check for review—See Below

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.2

HEARING DATE: December 8, 2016

CASE NUMBER: ZAP1037TH16 – Thermal Operating Company, LLC
(Representative: Fayres Hall, Albert A. Webb and Associates)

APPROVING JURISDICTION: County of Riverside

JURISDICTION CASE NO: PP26121 (Plot Plan)

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends a finding of CONSISTENCY for the Plot Plan, subject to the conditions included herein.

PROJECT DESCRIPTION: The applicant proposes to develop 14 industrial (predominately warehouse) 2-story buildings ranging from 19,400 to 64,500 square feet in area, with a cumulative total gross floor area of 361,800 square feet on 12.8 acres (14.16 acres gross) (Lots 1 through 14 of Parcel Map No. 36844) and an overflow 80-space parking lot on Lot 15.

The Commission had previously found the original proposal for a tentative parcel map to divide 15.8 acres into fifteen (15) numbered lots, plus two letter lots for a private street (Tower Road) and open space frontage, consistent via ZAP1031TH15.

PROJECT LOCATION: The project site is located easterly of Tyler Street and northerly of 62nd Avenue, along the west side of Ascot Drive (a private road), and southerly of Jasper Lane, in the unincorporated community of Thermal, approximately 4,280 feet southwesterly of the southerly terminus of Runway 17-35 at Jacqueline Cochran Regional Airport.

LAND USE PLAN: 2005 Jacqueline Cochran Regional Airport Land Use Compatibility Plan (last amended in 2006)

- a. Airport Influence Area: Jacqueline Cochran Regional Airport
- b. Land Use Policy: Airport Compatibility Zone C
- c. Noise Levels: Below 55 CNEL from aircraft at this location

BACKGROUND:

Non-Residential Average Intensity: Compatibility Zone C limits average intensity to 75 people per acre.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, the following rates were used to calculate the occupancy for the proposed building:

- Warehouse area – 1 person per 500 square feet
- Office area – 1 person per 200 square feet

Based on the site plan provided, the cumulative total of all 14 industrial buildings is 361,800 square feet of building area (307,530 square feet of warehouse area and 54,270 square feet of office), accommodating a total of 886 people on 14.16 gross acres. The resulting average intensity of 63 people per acre is consistent with the Compatibility Zone C average intensity criterion of 75 people per acre.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle in the absence of more precise data). Based on the number of parking spaces required (425 spaces), the total occupancy would be estimated at 638 people for an average acre intensity of 45 people per acre over the entirety of the site.

Non-Residential Single-Acre Intensity: Compatibility Zone C limits single-acre intensity to 150 people. Based on the site plan and floor plans provided for the project, the most intense single-acre would occur on Lots 13 and 14 in Compatibility Zone C. This single-acre area contains a combined total of 58,447 square feet of building area (8,767 square feet of office area and 49,680 square feet of warehouse area), resulting in an occupancy of 143 people, which is consistent with the Zone C single acre criterion of 150 people.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Zone C (Children's schools, day care centers, libraries, hospitals, nursing homes, buildings with more than 2 aboveground habitable floors, highly noise-sensitive outdoor non-residential uses, aboveground bulk storage of hazardous materials, critical community infrastructure facilities, and hazards to flight) within the project.

Noise: The property lies outside the areas subject to average exterior noise levels greater than 55 CNEL. Therefore, no special measures to mitigate aircraft-generated noise are required.

Part 77: The elevation of Runway 18-36 at its southerly terminus is approximately -134 feet (i.e., 134 feet below mean sea level). At a distance of approximately 4,280 feet from the runway, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding -91.2 feet mean sea level (MSL). The site elevation is approximately -144 feet MSL and

the maximum height of the buildings is 35 feet for a top point elevation of -109 feet MSL. Therefore, review of buildings by the FAA Obstruction Evaluation Service (FAA OES) is not required.

Open Area: Compatibility Zone C requires a minimum of 20% open area, which would require a minimum of 2.56 acres of open area. However, PP26121 is an implementing development related to PM36844 (ZAP1031TH15), where it was identified that open area for the larger Thermal Motorsports Park was reviewed pursuant to ZAP1024TH14 determining that the Thermal Club project on the whole provides 233.6 acres of open area.

CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting plans, if any, shall be transmitted to Riverside County Economic Development Agency – Aviation Division personnel and to the Jacqueline Cochran Regional Airport for review and comment. (Failure to comment within thirty days shall be considered to constitute acceptability on the part of the airport manager.)
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations, or any type of strobe light, toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, livestock operations, production of cereal grains, sunflower, and row crops, artificial marshes, wastewater management facilities, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children’s schools, day care centers, libraries, hospitals, nursing homes, places of worship, highly noise-sensitive outdoor non-residential uses, and hazards to flight.

Staff Report

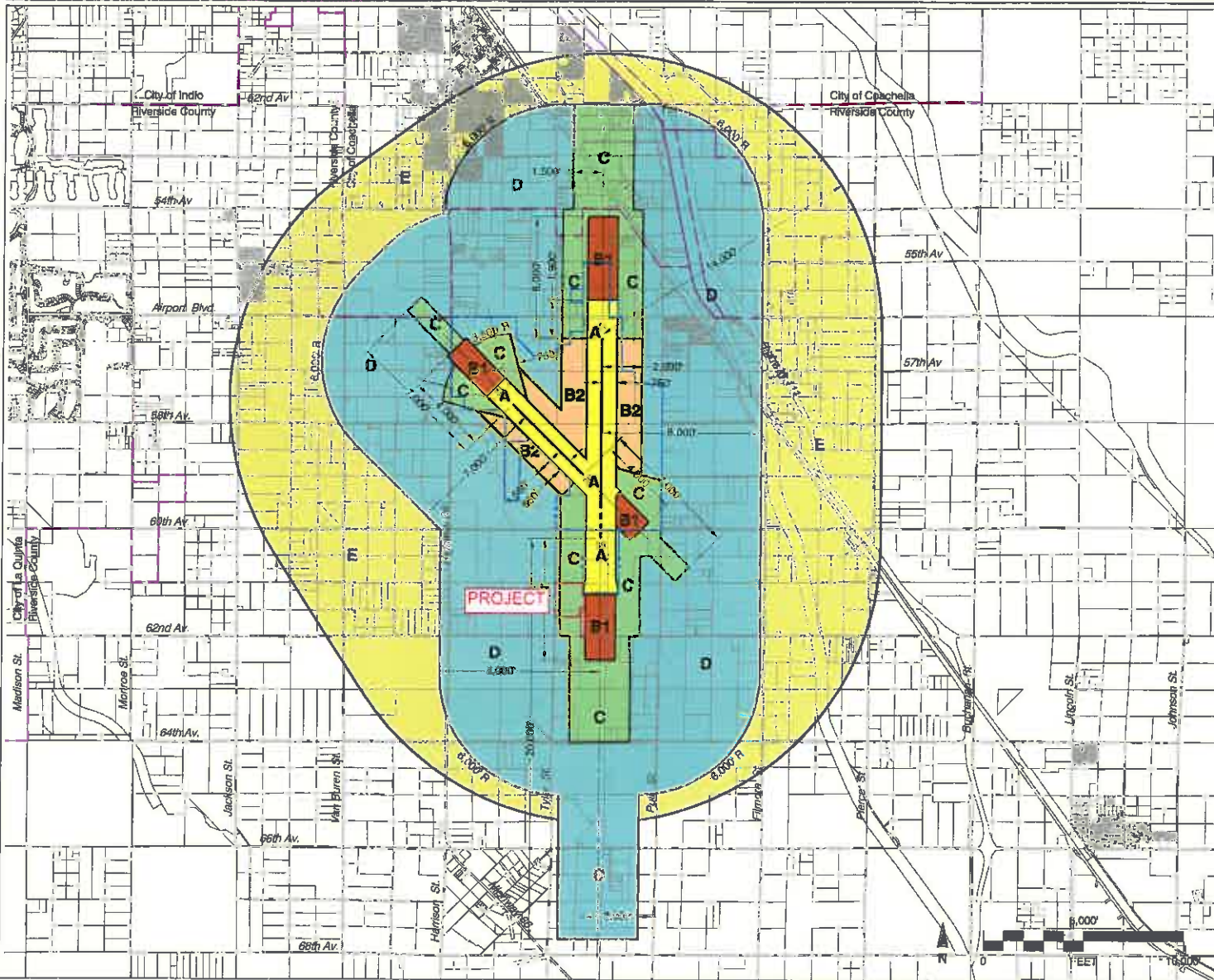
Page 4 of 4

3. The attached notice shall be provided to all prospective purchasers of the property and tenants of the buildings thereon, and shall be recorded as a deed notice.
4. Any new detention basin(s) shall be designed so as to provide a maximum 48-hour detention period for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basin that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
5. A finding of consistency for this project is based on the proposed office and warehouse use. Any other subsequent use will be subject to ALUC review.

Y:\AIRPORT CASE FILES\UCRA\ZAP1037TH16\ZAP1037TH16sr.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Legend

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E

Boundary Lines

- Airport Property Line - Existing
- Airport Property Line - Planned
- City Limits

Note

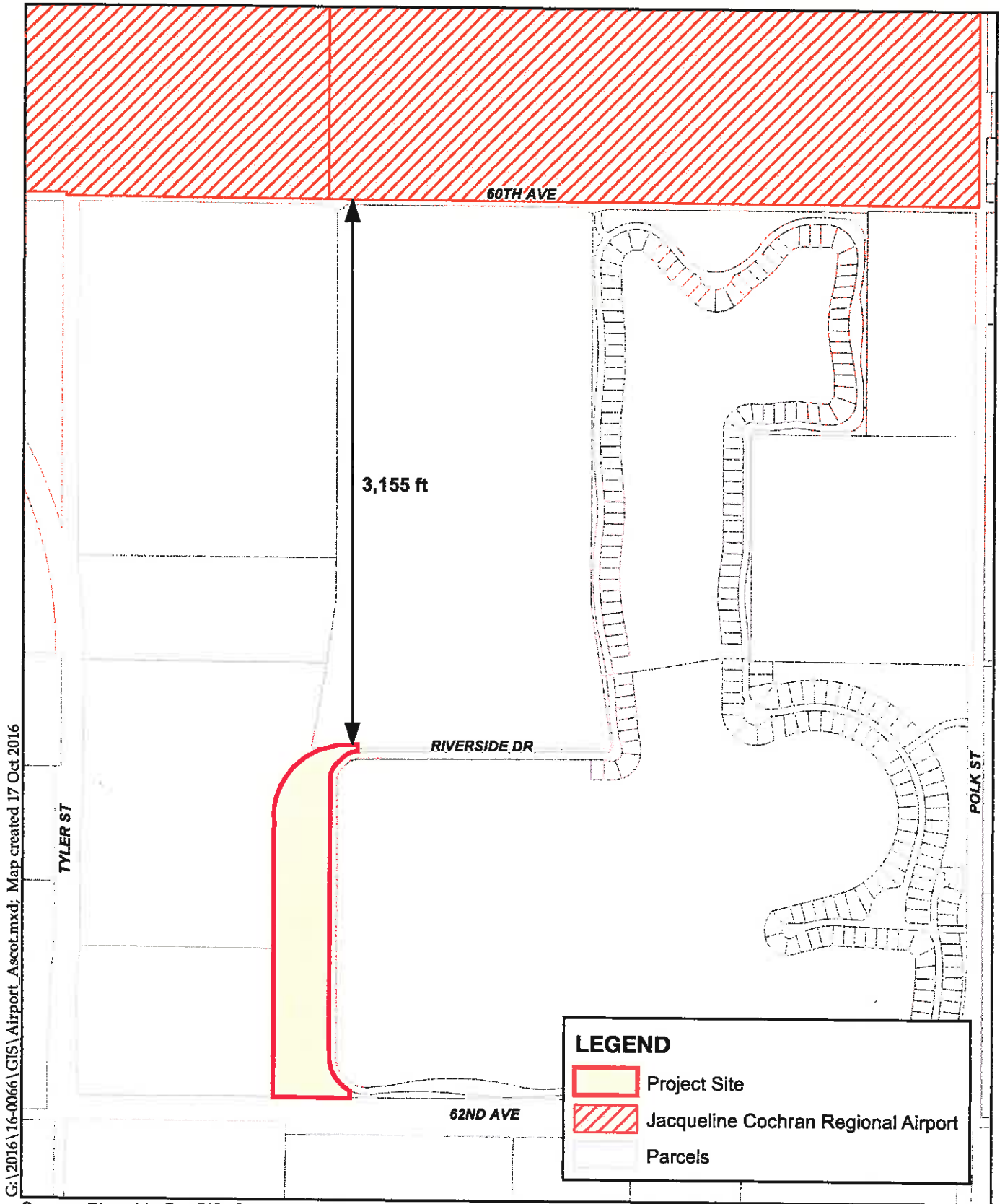
Except for southern extension, Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

See Chapter 2, Table 2A for compatibility criteria associated with this map.

Riverside County
Airport Land Use Commission
Riverside County
Airport Land Use Compatibility Plan
Policy Document
 (Adopted June 2005)

Map JC-1

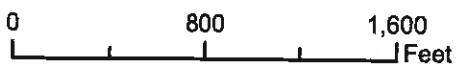
Compatibility Map
Jacqueline Cochran Regional Airport



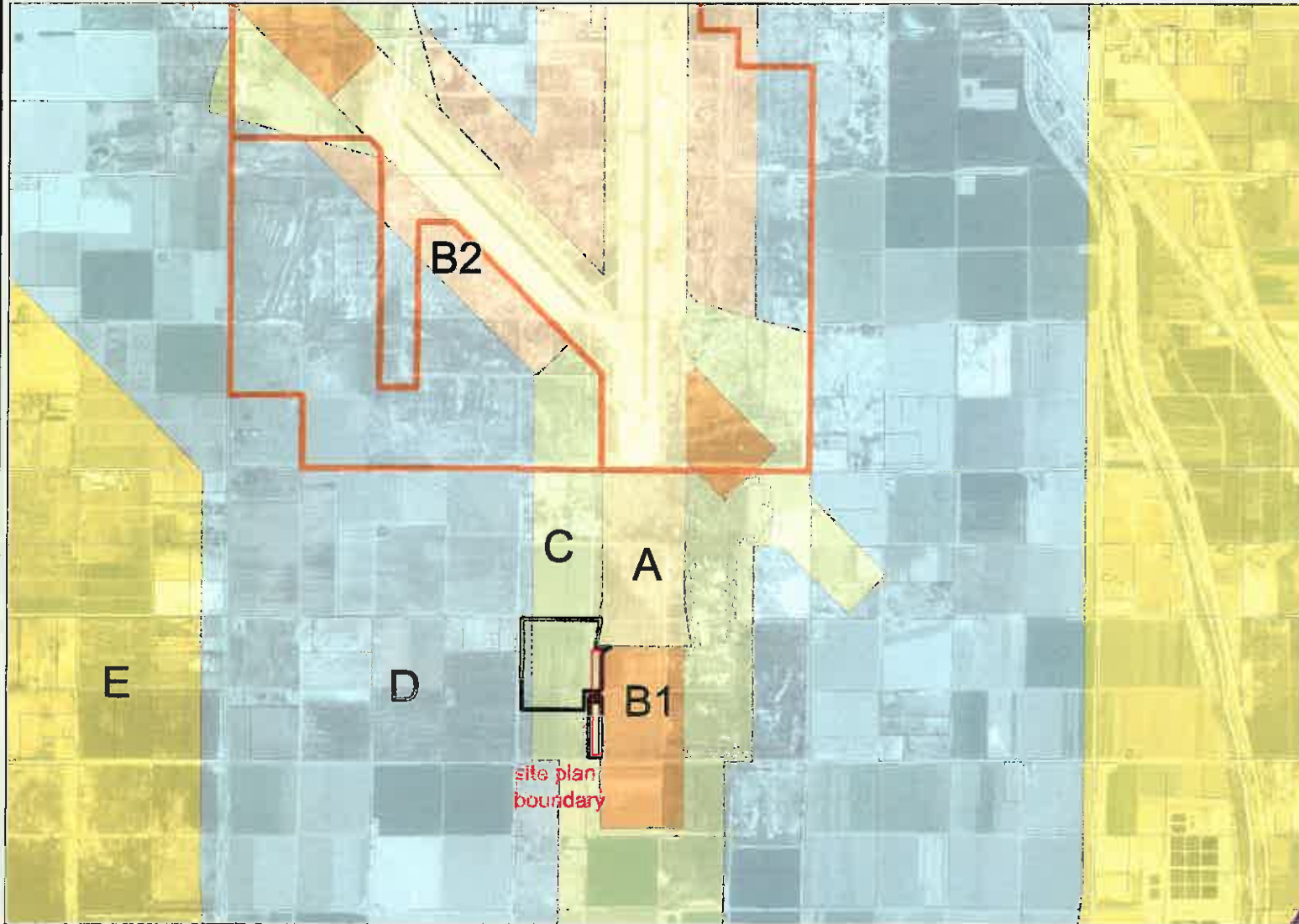
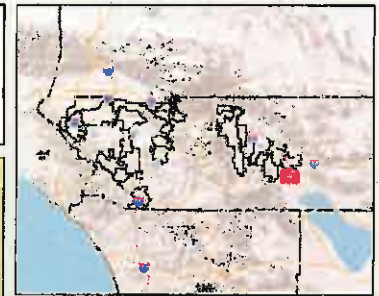
G:\2016\16-0066\GIS\Airport_Ascot.mxd; Map created 17 Oct 2016

Source: Riverside Co. GIS, Oct. 2016.

Relation to Nearby Airport Ascot



My Map



Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-FXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



0 3,373 6,746 Feet



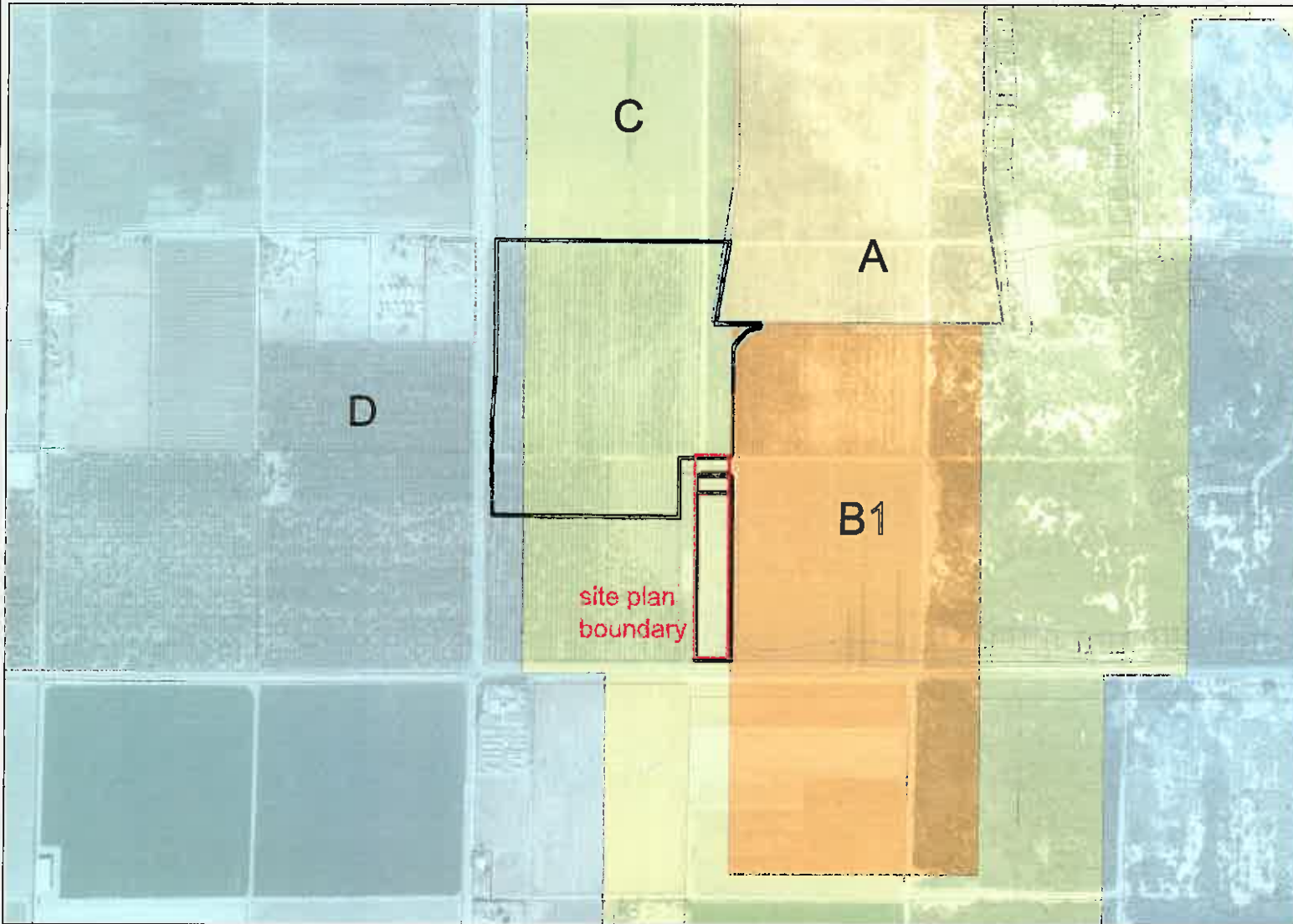
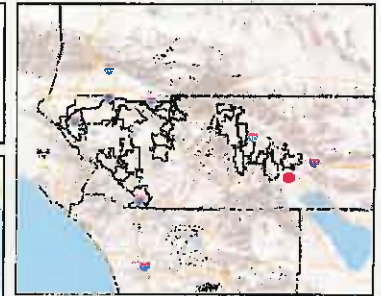
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/9/2016 10:33:10 AM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



0 1,116 2,231 Feet



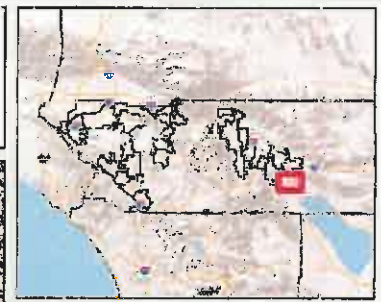
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/9/2016 10:28:04 AM










© Riverside County RCIT GIS

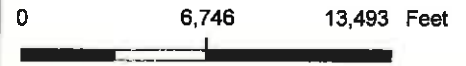
Notes

My Map



Legend

-  City Boundaries
-  Cities
- highways_large**
 -  HWY
 -  INTERCHANGE
 -  INTERSTATE
 -  USHWY
-  majorroads
-  counties
-  cities



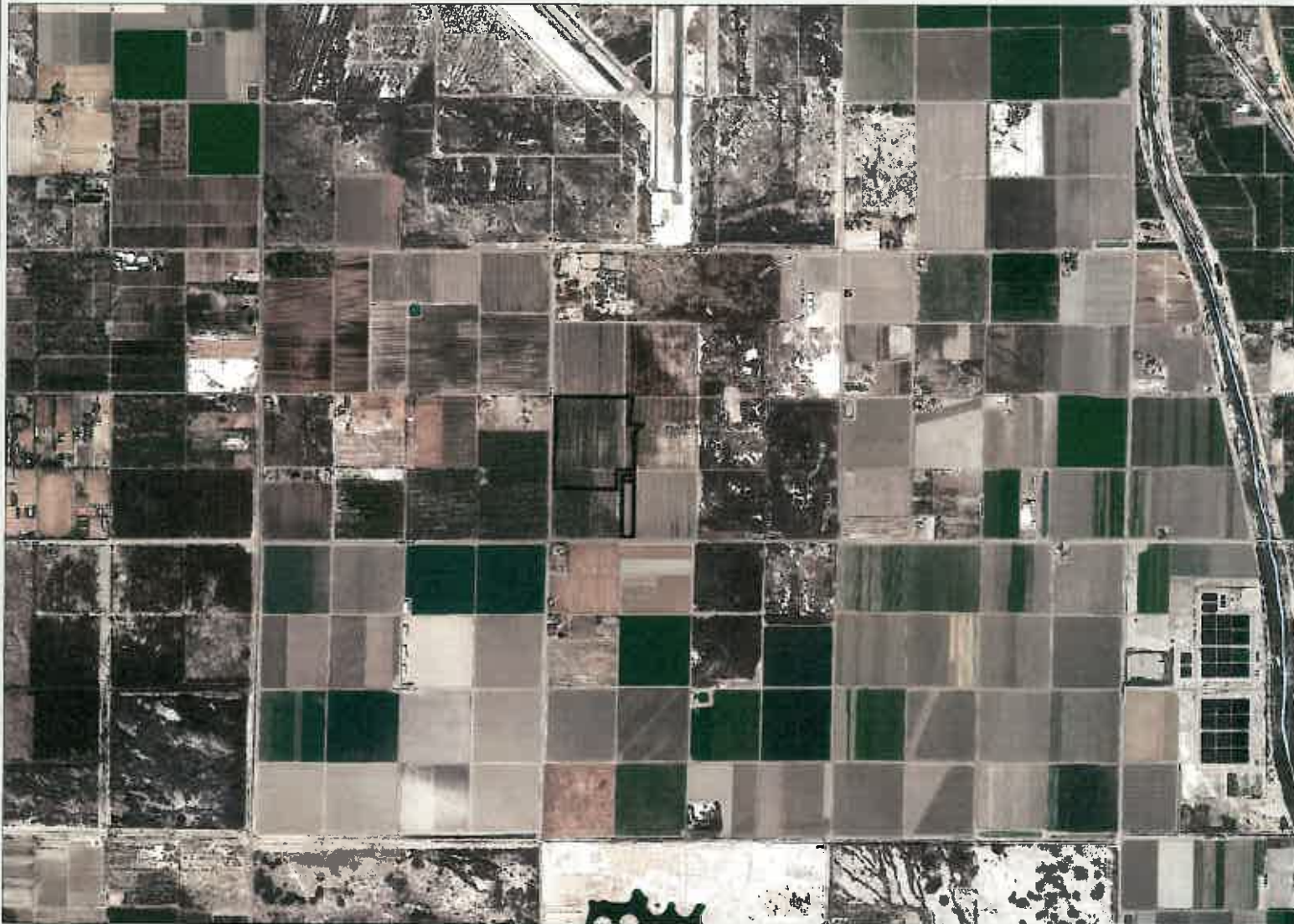
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/9/2016 10:33:59 AM

© Riverside County RCIT GIS

Notes

My Map



Legend

- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads**
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities
- hydrographylines
- waterbodies**
- Lakes
- Rivers



0 3,373 6,746 Feet



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/9/2016 10:34:29 AM


















© Riverside County RCIT GIS

Notes

My Map



Legend

-  Display Parcels
-  City Boundaries
-  Cities
-  roadsanno
-  highways
-  HWY
-  INTERCHANGE
-  INTERSTATE
-  OFFRAMP
-  ONRAMP
-  USHWY
-  counties
-  cities
-  hydrographylines
-  waterbodies
-  Lakes
-  Rivers



0 1,687 3,373 Feet



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/9/2016 10:34:59 AM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFF-RAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 1,116 2,231 Feet



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON...11/9/2016 10:29:01 AM


















© Riverside County RCIT GIS

Notes

My Map



Legend

-  Display Parcels
-  City Boundaries
-  Cities
-  roads
-  highways
-  HWY
-  INTERCHANGE
-  INTERSTATE
-  OFFRAMP
-  ONRAMP
-  USHWY
-  counties
-  cities
-  hydrographylines
-  waterbodies
-  Lakes
-  Rivers



0 843 1,687 Feet



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/9/2016 10:35:26 AM

© Riverside County RCIT GIS

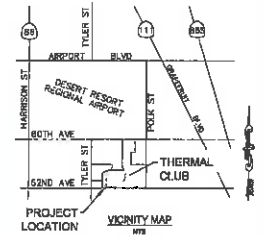
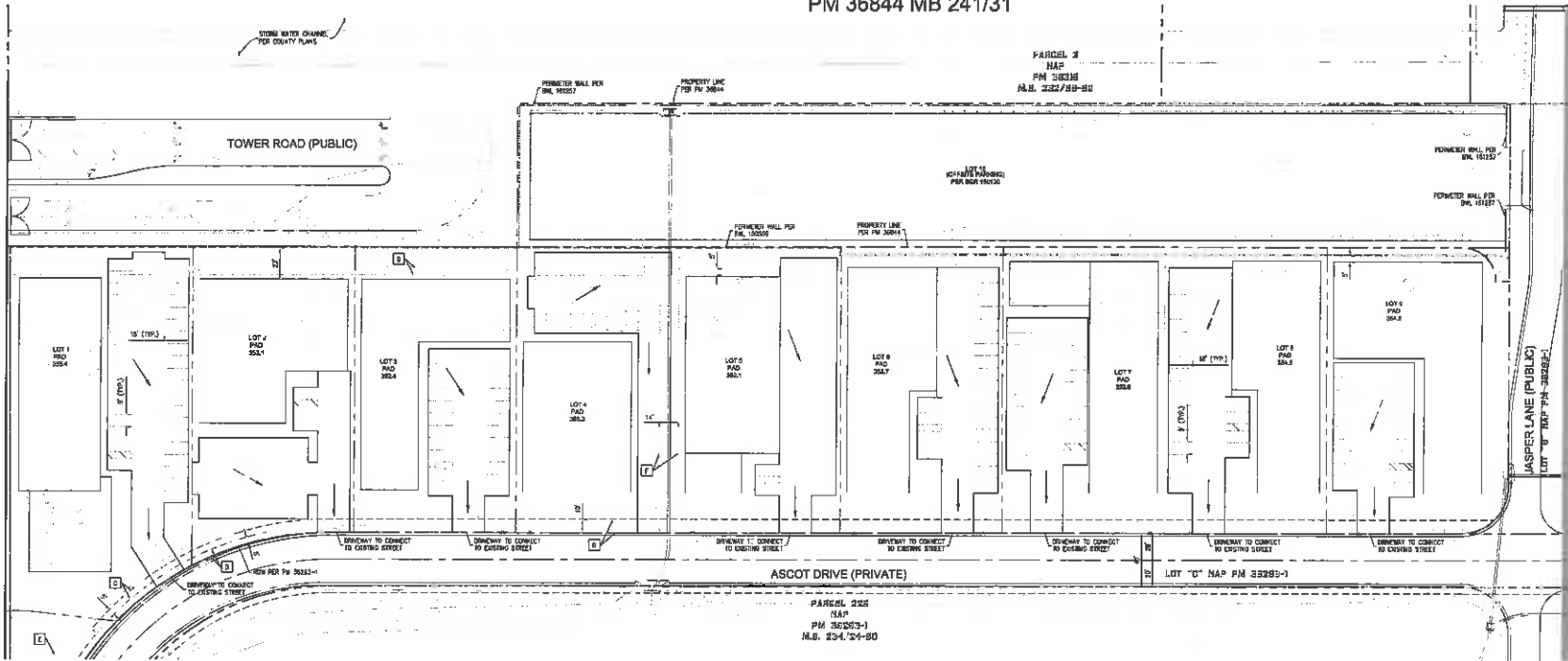
Notes

PLOT PLAN No. _____

ASCOT LOTS PLOT PLAN

THE THERMAL CLUB

PM 36844 MB 241/31



LEGAL DESCRIPTION
 BEING A PART OF PM 36844, FURTHER DESCRIBED AS PARCELS 1-15 OF PARCEL MAP NO. 36844 UNB 241/31 IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA.

PROJECT DESCRIPTION
 PLOT PLAN FOR APPROVAL OF CONSTRUCTION, BUILDING AND SITE LAYOUTS ALONG ASCOT DRIVE.

PROJECT DATA

APPLICANT
 THERMAL DEVELOPMENT COMPANY, LLC
 1845 W. TERRY STREET, SUITE 100
 THERMAL, CA 92581

CONTACT
 THERMALDEVELOPMENT.COM
 788 EAST CHURCH STREET
 1843 N. TERRY STREET, SUITE 100
 THERMAL, CA 92581
 (951) 498-1214

ENGINEER
 RCE CONSULTANTS, INC.
 24422 Avenida de la Carlota, Suite 200
 Laguna Hills, CA 92653
 (949) 433-2111
 RCECONSULTANTS.COM

ASSESSORS PARCEL NUMBER
 788-180-004 TRM 001
 788-180-004 TRM 002
 788-180-004 TRM 003
 788-180-004 TRM 004
 788-180-004 TRM 005
 788-180-004 TRM 006
 788-180-004 TRM 007
 788-180-004 TRM 008
 788-180-004 TRM 009
 788-180-004 TRM 010
 788-180-004 TRM 011
 788-180-004 TRM 012
 788-180-004 TRM 013
 788-180-004 TRM 014
 788-180-004 TRM 015

DATE OF PREPARATION
 SEPTEMBER 26, 2025

THERMAL DEVELOPMENT COMPANY
 1845 W. TERRY STREET, SUITE 100
 THERMAL, CA 92581

EXISTING ZONING
 SPECIFIC PLAN NO. 300, BEC 1004
 PLANNED ZONING ACT, P.L.C. 1-1
 HEAVY INDUSTRIAL/COMMERCIAL, RETAIL

PROPOSED ZONING
 SPECIFIC PLAN NO. 300, BEC 1004
 PLANNED ZONING ACT, P.L.C. 1-1
 HEAVY INDUSTRIAL/COMMERCIAL, RETAIL

ADJACENT EXISTING LAND USE
 RESIDENTIAL

UTILITY FURNISHINGS
 WATER: CONCELA VALLEY WATER
 SEWER: CONCELA VALLEY WRPD
 GAS: CONCELA VALLEY WRPD
 ELECTRICITY: CALPINE
 TELEPHONE: THE BIX COMPANY
 CABLE TV: BUREAU OF PUBLIC UTILITIES DISTRICT
 VIDEO: BUREAU OF PUBLIC UTILITIES DISTRICT
 SIGNAL: PUBLIC UTILITIES DISTRICT

COUNTY SERVICE AREA
 RIVERSIDE COUNTY

SCHOOL CONTACT
 CONCELA VALLEY UNITED SCHOOL DISTRICT

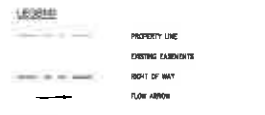
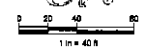
SHEET INDEX
 1. PLOT PLAN LOTS 1-6, 15
 2. PLOT PLAN LOTS 7-14

MATCHLINE - SEE SHEET 2

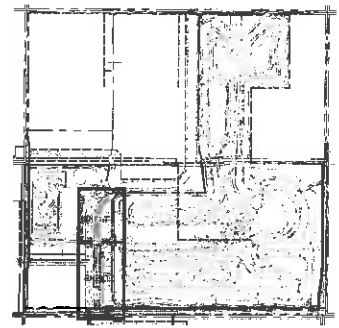
ASCOT DRIVE - ALUC OCCUPANCY & RIVERSIDE COUNTY PARKING REQUIREMENTS

LOT #	ACRES	TOTAL BUILDING AREA (SF)	OCCUPANCY TYPE	AREA (SF) PER OCC. TYPE	PER ALUC OCC. TYPE	ALUC LOAD	TOTAL ALUC OCC. LOAD	ALUC MAX OCCUPANCY ALLOWED	PARKING REQUIRED	TOTAL PARKING REQUIRED	PARKING PROVIDED	OFFSITE PARKING PROVIDED	ADA PARKING PROVIDED
1	0.6	2340	WAREHOUSE	1980	38.8	57.3	57.3	9.9	27.5	28	0	0	0
2	0.6	2500	OFFICE	2010	17.8	24.3	24.3	17.8	27.5	28	0	0	0
3	0.6	1950	WAREHOUSE	1640	30.9	47.5	47.5	14.6	22.8	23	0	0	0
4	0.6	1800	WAREHOUSE	1480	27.2	47.5	47.5	14.6	22.8	18	4	0	0
5	0.6	1800	WAREHOUSE	1480	27.2	47.5	47.5	14.6	22.8	21	2	0	0
6	0.6	1800	WAREHOUSE	1480	27.2	47.5	47.5	14.6	22.8	22	1	0	0
7	0.6	1800	WAREHOUSE	1480	27.2	47.5	47.5	14.6	22.8	23	0	0	0
8	0.6	1800	WAREHOUSE	1480	27.2	47.5	47.5	14.6	22.8	22	0	0	0
9	0.6	1800	WAREHOUSE	1480	27.2	47.5	47.5	14.6	22.8	22	0	0	0
10	0.6	1800	WAREHOUSE	1480	27.2	47.5	47.5	14.6	22.8	22	0	0	0
11	0.6	1800	WAREHOUSE	1480	27.2	47.5	47.5	14.6	22.8	22	0	0	0
12	0.6	1800	WAREHOUSE	1480	27.2	47.5	47.5	14.6	22.8	22	0	0	0
13	0.6	1800	WAREHOUSE	1480	27.2	47.5	47.5	14.6	22.8	22	0	0	0
14	0.6	1800	WAREHOUSE	1480	27.2	47.5	47.5	14.6	22.8	22	0	0	0
15	0.6	1800	WAREHOUSE	1480	27.2	47.5	47.5	14.6	22.8	22	0	0	0
OFFSITE PARKING											20	0	

- NOTES:**
- THE NUMBER SHOWN IN THIS TABLE FOR TOTAL BUILDING AREA AND TOTAL PARKING PROVIDED IS BASED ON THE SITE PLAN LAYOUT THAT IS PART OF THIS SUBMITTAL. THE NUMBER IS BASED ON A BUILDING WITH 10% OF THE TOTAL SQ. FT. TO BE OFFICE SPACE AND 90% OF THE TOTAL SQ. FT. TO BE WAREHOUSE SPACE. FURNISHINGS OF THE USE OF THE BUILDING OR LAYOUT ON THE SITE, THE TOTAL OCCUPANCY OF THE BUILDING CANNOT EXCEED COLUMN 9 AND TOTAL REQUIRED PARKING MUST BE PROVIDED.
 - ARCHITECTURAL STYLE FOR ALL STRUCTURES TO BE CONTEMPORARY OR SPANISH REVIVAL PER APPROVED PROJECT DESIGN GUIDELINES.
 - OFFSITE PARKING LOCATED ON LOT 15.
 - BUILDING SETBACKS AND HEIGHTS PER SPECIFIC PLAN 3PA AND THERMAL CLUB DESIGN GUIDELINES.
 - FINAL BUILDING OCCUPANCY, CONSTRUCTION TYPE AND USES PER APPROVED SPECIFIC PLAN AND ZONING.
 - NO OVERNIGHT STAYS WILL BE PERMITTED.



- NOTES:**
- ELEMENTS OF RECORD ARE AS SHOWN ON THIS SHEET.
 - NO SURFACE OR SUBSURFACE UTILITIES, EXCEPT AS SHOWN, ARE TO BE LOCATED ON THIS PROPERTY.
 - NO USES ARE TO BE LOCATED ON THIS PROPERTY.
 - LAND IS IN HIGH LIQUEFACTION POTENTIAL AREA AND IS NOT WITHIN A FAULT HAZARD ZONE.
 - LAND IS NOT SUBJECT TO OVERFLOW, INUNDATION OR FLOOD HAZARD.
 - FLOOD ZONE X AREA AS SHOWN ON FEMA PANEL NO. 080603050C OFFICIAL SITE MAPS OF 2004.
 - THE PROJECT WILL COMPLY WITH ALL APPLICABLE REQUIREMENTS BY RETAINING THE EXISTING 10'-0" HIGH, 24" WIDE VISION SIGN ON-SITE.
 - SETBACKS AND LANDSCAPE PLANTING TO BE DECREASED TO A LATER TIME.
 - RELANDSCAPING AND LAND USES PER SPECIFIC PLAN NO. 300.
 - THIS PROJECT HAS PASSED APPROVAL UNDER PIA 241-1.
 - DRAINAGE FOR THE THERMAL CLUB HYDROLOGY AND HYDRAULICS STUDY DATED APRIL 2024 IS ASSUMED.



REVISIONS	DATE/NO.

RCE
CONSULTANTS, INC.

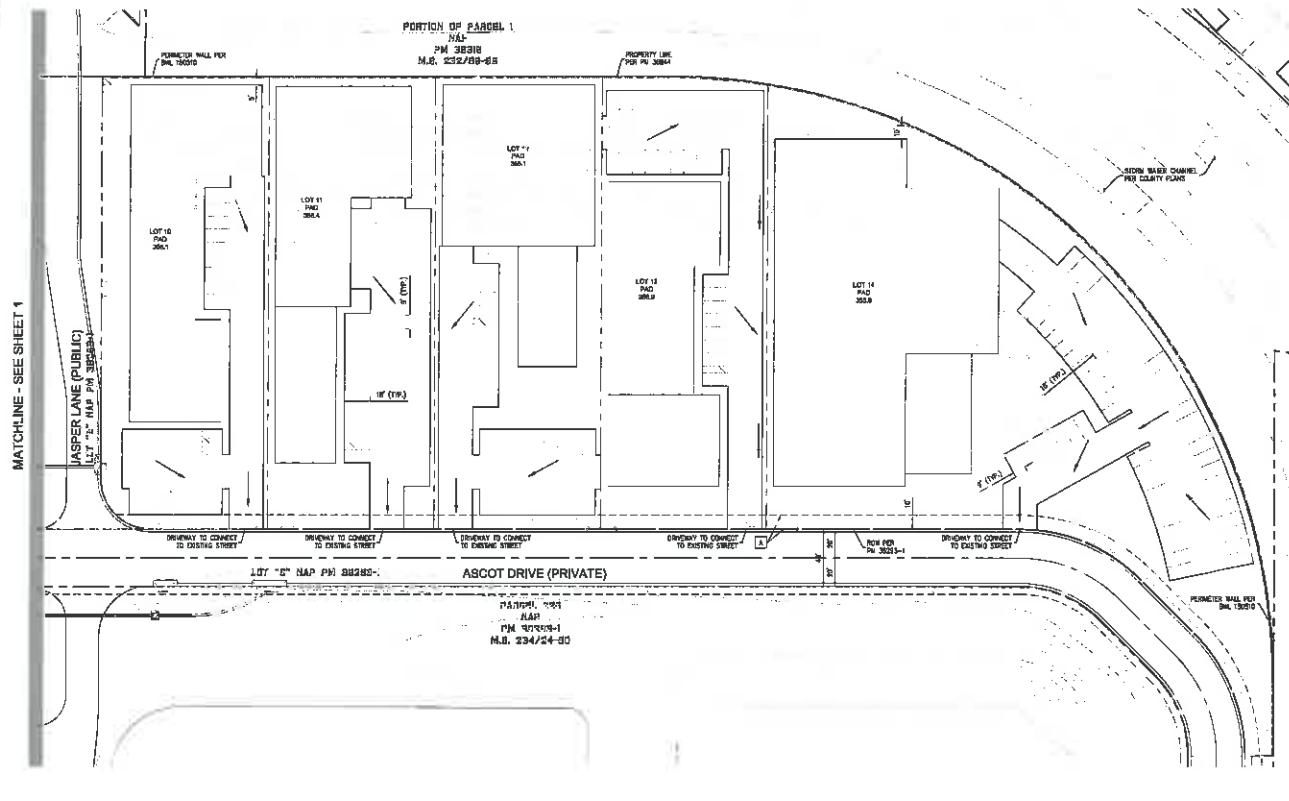
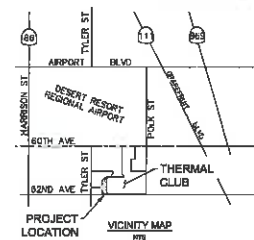
24422 Avenida de la Carlota Suite 200
Laguna Hills, CA 92653
Phone: 949.453.0111

SIGNATURE _____
FE _____
EXP. _____

STAMP	BASIS OF BEARINGS BEARINGS ARE BASED UPON THE NORTHERLY LINE OF THE NORTHWEST ONE-QUARTER OF SECTION 33, TOWNSHIP 8 SOUTH, RANGE 8 EAST, SERIAL 15, BEING 186.917% AS SHOWN BY RECORDS OF SURVEY ON FILE IN BOOK 86 AT PAGES 18 THROUGH 41 THEREOF, RECORDS OF RIVERSIDE COUNTY, CALIFORNIA.	BENCH MARK CALIFORNIA B.M. NO. RM-148.1 2-1/2" DIA. BRASS SET IN CONCRETE AT THE S.W. CORNER OF THE 50' X 50' PAVEMENT PLANT 1588 R-148.1, 0.3' BELOW GROUND AND 3' E.C. OF P.M. 1588.	APPROVED BY:	IN THE UNINCORPORATED TERRITORY OF THERMAL, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA PLOT PLAN No. ASCOT LOTS PLOT PLAN LOTS 1-9, 15 THE THERMAL CLUB PM 36844 MB 241/31	DRAWING NAME: Ascot Lots Plot Plan PROJECT No. SHEET 1 OF 2
--------------	--	--	---------------------	--	--

PLOT PLAN No. _____

ASCOT LOTS PLOT PLAN THE THERMAL CLUB PM 36844 MB 241/31



ASCOT DRIVE - ALUC OCCUPANCY & RIVERSIDE COUNTY PARKING REQUIREMENTS

LOT #	ACRES	TOTAL BUILDING AREA (SQ FT)	OCCUPANCY TYPE	AREA (SQ FT) PER ALLOC OCC. LOAD PER OCC. TYPE	TOTAL ALLOC. OCC. LOAD	ALUC MAX OCCUPANCY ALLOWED	PARKING REQUIRED	TOTAL PARKING PROVIDED	OFFSITE PARKING PROVIDED	ADD. PARKING PROVIDED
10	0.8	30500	WAREHOUSE	20783	51.8	74.3	18.8	12.8	35.8	13
			OFFICE	4841	23.7			23.7		
11	0.9	17800	WAREHOUSE	23482	48.9	67.6	67.7	11.2	32.4	1
			OFFICE	4163	20.3			20.3		
12	0.9	23600	WAREHOUSE	22483	45.9	67.6	67.7	11.3	32.4	2
			OFFICE	4145	20.2			20.2		
13	0.8	27300	WAREHOUSE	22395	46.4	64.9	67.1	11.6	32.1	4
			OFFICE	4095	20.3			20.3		
14	1.0	81500	WAREHOUSE	84823	108.7	150.0	150.5	75.4	75.8	54
			OFFICE	8013	40.4			40.4		

- NOTES:**
- THE NUMBERS SHOWN IN THE MATRIX FOR TOTAL BUILDING AREA (SQ FT) AND TOTAL PARKING PROVIDED (SQ FT) ARE BASED ON THE SITE PLAN LAYOUT THAT IS PART OF THIS SUBMITTAL. THE MATRIX IS BASED ON A BUILDING WITH 10% OF THE TOTAL BUILDING AREA BEING OFFICE SPACE AND 90% OF THE TOTAL BUILDING AREA BEING WAREHOUSE SPACE. REDUCTIONS OF THE USE OF THE BUILDING OR LAYOUT OF THE SITE, THE TOTAL OCCUPANCY OF THE BUILDING CANNOT EXCEED COLUMN 7 AND TOTAL REQUIRED PARKING MUST BE MET. SEE 1.
 - ARCHITECTURAL STYLE FOR ALL STRUCTURES TO BE CONTEMPORARY OR BARNER REVEAL PER APPROVED PROJECT DESIGN GUIDELINES.
 - OFFSITE PARKING LOCATED 0.75 MI. OF 10.
 - BUILDING SETBACKS AND HEIGHTS PER SPECIFIC PLAN (SP) AMENDMENT NO. 3 AND THERMAL CLUB DESIGN GUIDELINES.
 - FINAL BUILDING OCCUPANCY, CONSTRUCTION TYPE, AND USES PER APPROVED SPECIFIC PLAN AMENDMENT NO. 3.
 - NO OVERNIGHT STAYS WILL BE PERMITTED.

EXISTING EASEMENTS

SYMBOL ON MAP	DESCRIPTION
A	10' PUBLIC UTILITY EASEMENT IN FAVOR OF THE COUNTY OF RIVERSIDE RECORDED DECEMBER 01, 2014 AS DOCUMENT NO. 2014-048030.
B	10' PUBLIC UTILITY EASEMENT IN FAVOR OF THE COUNTY OF RIVERSIDE RECORDED DECEMBER 01, 2014 AS DOCUMENT NO. 2014-048031.
C	5' PUBLIC UTILITY EASEMENT IN FAVOR OF THE COUNTY OF RIVERSIDE RECORDED DECEMBER 01, 2014 AS DOCUMENT NO. 2014-048032.
D	5' PUBLIC UTILITY EASEMENT PER PARCEL MAP NO. 30923-1, PAGE 234/24-25.
E	PORTION OF TRAIL EASEMENT BEING USED FOR SUBSTATION EASEMENT IN FAVOR OF CHANDLER IN THE DISTRICT PER PARCEL MAP NO. 30923-1, PAGE 234/24-25.
F	14' PRIVATE SUBSURFACE ORANGE EASEMENT FOR IRRIGATION NO. 2014-048113 RECORDED ON SEPTEMBER 27, 2014.
G	5' PRIVATE SUBSURFACE TIE DRAIN EASEMENT FOR IRRIGATION NO. 2014-048113 RECORDED ON SEPTEMBER 27, 2014.

DENSITY CALCULATIONS

TOTAL AREA (PERFORM PLOT PLAN AREA)	12.60 AC
PAVED AREA	4.91 AC (39.0%)
SEALING SURFACE AREA	4.35 AC (34.5%)
LANDSCAPE AREA	3.34 AC (26.5%)

CHRONOLOGICAL ORDER OF APPROVAL

DOCUMENT NAME	PERMIT PLAN NO.	DATE
ASCOT DRIVE MAP 3033	SP3003	AUG. 2002
ASCOT DRIVE MAP 3033	SP3003	AUG. 2002
PLOT PLAN 24890	PP248900	MARCH 2011
TERRACE PARCEL MAP 30215 - SCHEDULE "Y"	PM30215	MARCH 2011
TERRACE PARCEL MAP 30225 - SCHEDULE "Y"	PM30225	MARCH 2011
PARCEL MAP 30515	PM30515	SEPT. 2011
UNIT PARKING MAP	UPM-PM30225	APRIL 2012
PLOT PLAN SUBSTANTIAL CONFORMANCE 24890	PP2489005	MAY 2012
PARCEL MAP 30515-1	PM30515-1	OCT. 2012
REVISED PLOT PLAN NO. 24890R	PP24890R	APRIL 2013
PLOT PLAN SUBSTANTIAL CONFORMANCE 24890R	PP24890R5	JULY 2014
PLOT PLAN SUBSTANTIAL CONFORMANCE 24890R	PP24890R5	MARCH 2014



REVISIONS	DATE	NO.

RCE CONSULTANTS, INC.

24422 Avenida de la Cortada
Suite 200
Laguna Hills, Ca. 92653
Phone: 849.453.0111

SIGNATURE _____ DATE _____

STAMP	BASIS OF BEARINGS BEARINGS ARE BASED UPON THE NORTH-SOUTH LINE OF THE INTERSECTION CORNER OF SECTION 33, TOWNSHIP 8 SOUTH, RANGE 8 EAST, E3/4M, AS BEING REPRESENTED AS SHOWN BY RECORD OF SURVEY ON FILE IN BOOK 88 AT PAGE 30 THEREIN IN THE PUBLIC RECORDS OF RIVERSIDE COUNTY, CALIFORNIA.	BENCH MARK CALTRANS BU. NO. RM-148.1 2-1/2" BRASS BOLT IN CONCRETE AT THE CORNER OF HWY 60 AND AVE. D, STAMPED CALIFORNIA DEPT. OF TRANSPORTATION TRM 8-14.8 L. 0.3' BELOW GROUND AND 5' E.E. OF PM 307980 ELEVATION = 378.53 USCGS DATUM +100' 1972 ADJUSTMENT	APPROVED BY:
--------------	--	---	---------------------

IN THE UNINCORPORATED TERRITORY OF THERMAL, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA

PLOT PLAN No. _____
ASCOT LOTS PLOT PLAN
LOTS 10-14
THE THERMAL CLUB
PM 36844 MB 241/31

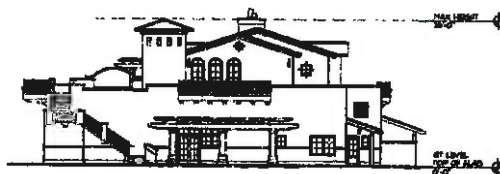
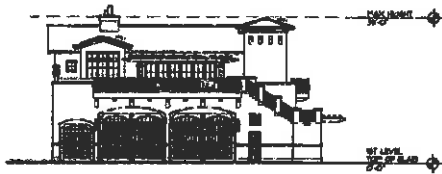
DRAWING NAME: Ascot Lots Plot Plan
PROJECT No.
SHEET 2 OF 2



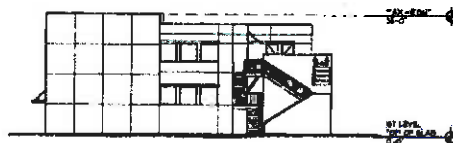
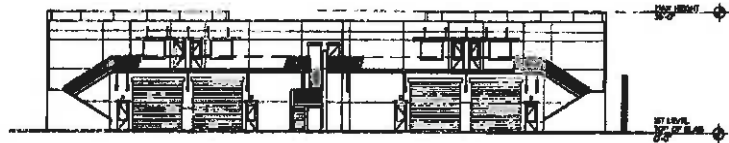
SCHEMATIC ELEVATION IMAGERY - CONTEMPORARY



SCHEMATIC ELEVATION IMAGERY - SPANISH REVIVAL



SCHEMATIC ELEVATION - SPANISH REVIVAL



SCHEMATIC ELEVATIONS - CONTEMPORARY

514520067Thermal - Abbot Lane Lots 16-40933 Working Drawings\A1 Site Aug-Sep 24, 2015



Revision table with columns for No., Date, and Revision.

Project Number: 16-409

The Thermal Club
ABBOT LANE LOTS
THE THERMAL CLUB
VENICE, CA

ISSUED FOR REVIEW
ALL NEW DRAWINGS SHALL BE APPROVED BY THE ARCHITECT AND ENGINEER
DATE: 09/24/15
DRAWN BY: [Name]
CHECKED BY: [Name]
DATE: September 24, 2015

Sheet Title
SITE PLAN

Sheet Number
A1.1

Lot	Net Area (Sq Ft)	Gross Area (Sq Ft)	Gross Area (Acres)	Avg Criteria	Max Occupancy
1	32,372	33,392	0.767	75	57.49
2	26,175	28,560	0.656	75	49.17
3	25,339	27,740	0.637	75	47.76
4	25,339	27,740	0.637	75	47.76
5	25,339	27,740	0.637	75	47.76
6	25,339	27,740	0.637	75	47.76
7	25,339	27,740	0.637	75	47.76
8	25,339	27,740	0.637	75	47.76
9	28,244	41,609	0.955	75	71.64
10	36,845	54,620	1.254	75	94.04
11	37,000	39,333	0.903	75	67.72
12	37,000	39,333	0.903	75	67.72
13	36,661	38,973	0.895	75	67.10
14	83,873	91,956	2.111	75	158.33
15	77,910	82,680	1.898	75	142.36

NOTICE OF PUBLIC HEARING

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Thursday from 8:00 a.m. to 5:00 p.m., except Thursday, November 24 (Thanksgiving Day) and by prescheduled appointment on Friday, December 2 from 8:30 a.m. to 5:00 p.m.

ATTENTION: ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. The County of Riverside may hold hearings on this project and should be contacted on non-ALUC issues.

PLACE OF HEARING: Eastern Municipal Water District
 2270 Trumble Road
 Perris, California

DATE OF HEARING: December 8, 2016

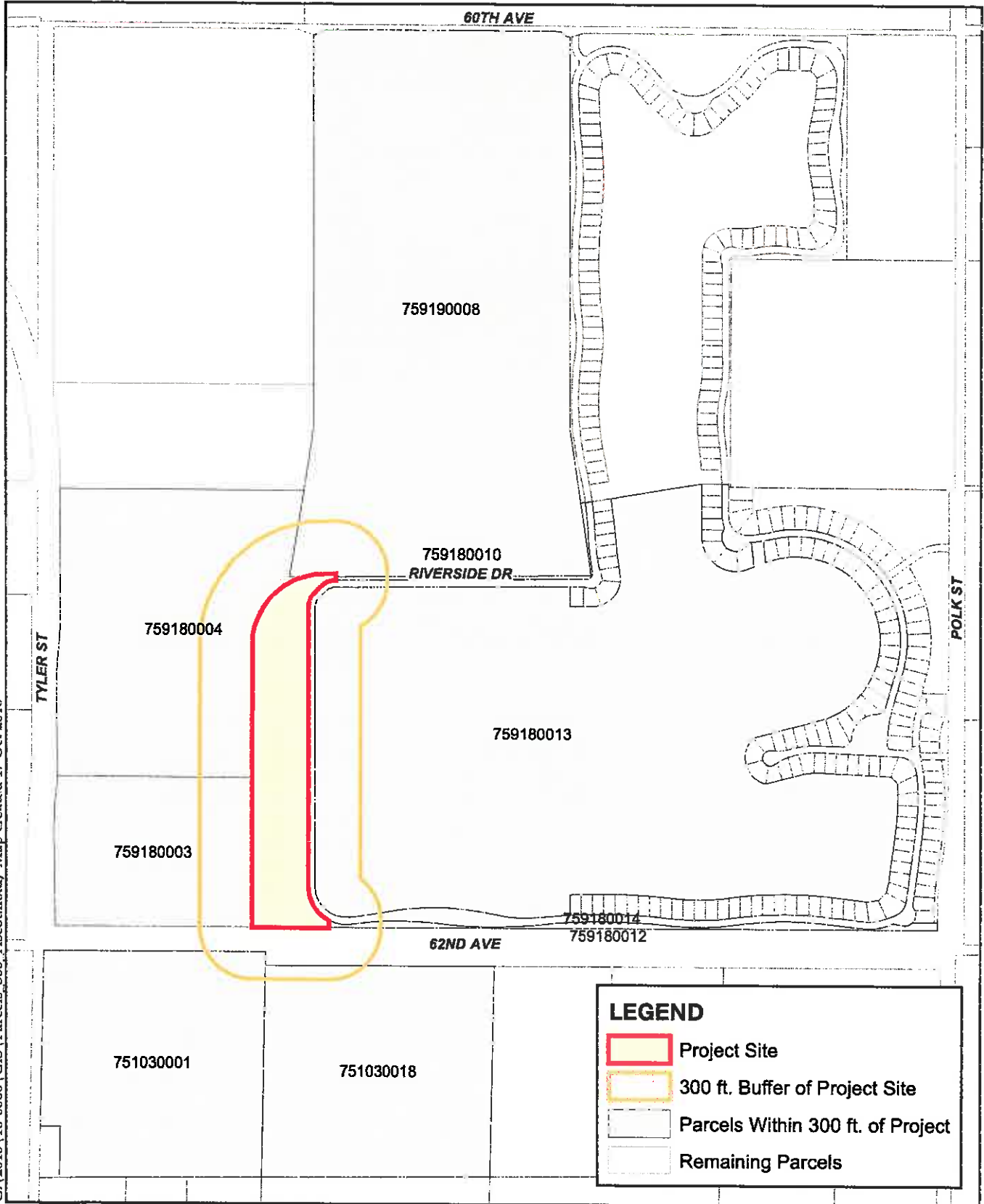
TIME OF HEARING: 9:00 A.M.

CASE DESCRIPTION:

ZAP1037TH16 – Thermal Operating Company, LLC (Representative: Fayres Hall, Albert A. Webb and Associates) – County of Riverside Planning Case No. PP26121 (Plot Plan). The applicant is proposing to develop 14 industrial (predominately warehouse) 2-story buildings ranging from 19,400 to 64,500 square feet in area, with a cumulative total gross floor area of 361,800 square feet on 12.8 acres (Lots 1 through 14 of Parcel Map No. 36844) and an overflow 80-space parking lot on Lot 15, all located easterly of Tyler Street and northerly of 62nd Avenue, along the west side of Ascot Drive (a private road), and southerly of Jasper Lane. (Airport Compatibility Zone C of the Jacqueline Cochran Regional Airport Influence Area)

FURTHER INFORMATION: Contact Paul Rull at (951) 955-6893. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Mr. Russell Brady of the County of Riverside Planning Department at (951) 955-3025.

C:\2016\16-0066\GIS\Parcels_300_Ascot.mxd; Map created 17 Oct 2016

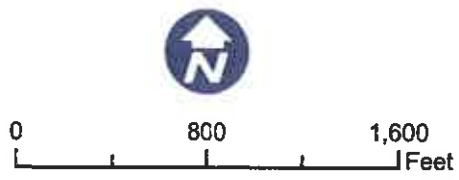


LEGEND

- Project Site
- 300 ft. Buffer of Project Site
- Parcels Within 300 ft. of Project
- Remaining Parcels

Source: Riverside Co. GIS, Oct. 2016.

Parcels Within 300 ft. of Project Ascot



**APPLICATION FOR MAJOR LAND USE ACTION REVIEW
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**

ALUC Identification No.

ZAP 1037 TH 16

PROJECT PROPONENT (TO BE COMPLETED BY APPLICANT)

Date of Application _____

Property Owner JTM Land Company, LLC c/o Tim Rogers Phone Number 310-453-0111

Mailing Address 1983 W. 190th Street, Suite 100
Torrance, CA 90504

Agent (if any) Albert A Webb Associates Phone Number 951-686-1070

Mailing Address 3788 McCray Street
Riverside, CA 92506

PROJECT LOCATION (TO BE COMPLETED BY APPLICANT)

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address North of Avenue 62, south of Avenue 60, east of Tyler Street and west of Polk Street

Assessor's Parcel No. Portion of APN 759-180-004, **005, 008, 009, 012** Parcel Size 12.60 with in Plot Plan

Subdivision Name TPM36844

Lot Number Lots 1-15 Zoning Classification Kohl Ranch Specific Plan #303

PROJECT DESCRIPTION (TO BE COMPLETED BY APPLICANT)

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe) Vacant previously disturbed.

Proposed Land Use (describe) Plot Plan to include Lots 1-15 consisting of one building per lot on 14 lots and the remaining lot for parking.

For Residential Uses Number of Parcels or Units on Site (exclude secondary units) N/A

For Other Land Uses Hours of Use Accessible 24 hours/ 7 days a week.

(See Appendix C) Number of People on Site Maximum Number 465.1

Method of Calculation Maximum Occupancy

Height Data Height above Ground or Tallest Object (including antennas and trees) 35' Max ft.

Highest Elevation (above sea level) of Any Object or Terrain on Site 355.8 **(-144.2)** ft.

Flight Hazards Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight? Yes No

If yes, describe _____

JCRA
Zone C

REFERRING AGENCY (APPLICANT OR JURISDICTION TO COMPLETE)

Date Received	<u>10/12/2016</u>	Type of Project	
Agency Name	<u>County of Riverside Planning Department</u>	<input type="checkbox"/> General Plan Amendment	
Staff Contact	<u>Planner not yet assigned</u>	<input type="checkbox"/> Zoning Amendment or Variance	
Phone Number	<u>951-955-3200</u>	<input type="checkbox"/> Subdivision Approval	
Agency's Project No.	<u>PP26121</u>	<input type="checkbox"/> Use Permit	
		<input type="checkbox"/> Public Facility	
		<input checked="" type="checkbox"/> Other	<u>Plot Plan</u>

A. NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. SUBMISSION PACKAGE:

ALUC REVIEW

- 1. Completed Application Form
- 1. Project Site Plan – Folded (8-1/2 x 14 max.)
- 1. Elevations of Buildings - Folded
- 1 Each . 8 ½ x 11 reduced copy of the above
- 1. 8 ½ x 11 reduced copy showing project in relationship to airport.
- 1 Set . Floor plans for non-residential projects
- 4 Sets. . Gummed address labels of the Owner and representative (*See Proponent*).
- 1 Set. . Gummed address labels of all property owners within a 300' radius of the project site. If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10), with ALUC return address.
- 4 Sets. . Gummed address labels of the referring agency (City or County).
- 1. Check for Fee (See Item "C" below)

STAFF REVIEW (Consult with ALUC staff planner as to whether project qualifies)

- 1. Completed Application Form
- 1. Project Site Plans – Folded (8-1/2 x 14 max.)
- 1. Elevations of Buildings - Folded
- 1. 8 ½ x 11 Vicinity Map
- 1 Set. . Gummed address labels of the Owner and representative (*See Proponent*).
- 1 Set . . Gummed address labels of the referring agency.
- 1. Check for review—See Below

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.3

HEARING DATE: December 8, 2016

CASE NUMBER: ZAP1020BA15 – RMG Residential 2010, LLLP
(Representative: Beau Cooper, United Engineering Group)

APPROVING JURISDICTION: City of Banning

JURISDICTION CASE NO: 15-70004 (Tentative Tract Map 36710)

MAJOR ISSUES: The proposed project results in a density of 4.3 dwelling units per gross acre or 4.5 dwelling units per net acre, which are inconsistent with the Compatibility Zone D minimum density criteria of less than 0.2 dwelling units per acre or greater than 5.0 dwelling units per acre. Compatibility Zone D also requires 1.06 acres of the total project site area for emergency landing open area purposes, with the project only providing 0.5 acres.

RECOMMENDATION: Staff recommends that the Commission find the Tentative Tract Map INCONSISTENT, based on the density being within the prohibited intermediate density range and not providing the required 10% acreage for open area. However, if the Commission is willing to consider application of Countywide Policy 3.3.6, it may find the Tentative Map CONSISTENT, subject to the conditions included herein.

PROJECT DESCRIPTION: The applicant is proposing to divide 10.67 acres into 46 single-family residential lots, one drainage basin lot, and one open space lot.

PROJECT LOCATION: The site is located on the northerly side of Wilson Street, westerly of Florida Street, southerly of Hoffer Street, and easterly of Alessandro Road in the City of Banning, approximately 4,930 feet northwesterly of Runway 8-26 at Banning Municipal Airport.

LAND USE PLAN: 2004 Banning Airport Land Use Compatibility Plan

- a. Airport Influence Area: Banning Municipal Airport
- b. Land Use Policy: Airport Compatibility Zone D
- c. Noise Levels: Below 55 CNEL noise contour from aircraft noise

BACKGROUND:

Residential Density: The site is located in Compatibility Zone D of the Banning Municipal Airport Influence Area. Compatibility Zone D restricts residential densities to either below 0.2 dwelling units per acre or above 5.0 dwelling units per acre. The overall gross acreage of the site is 10.67 acres (10.15 acres net), resulting in 4.3 dwelling units per acre (4.5 dwelling units per acre net), which would be inconsistent with the Compatibility Zone D criterion.

Prohibited and Discouraged Uses: The applicant does not propose any uses specifically prohibited or discouraged in Compatibility Zone D (highly noise-sensitive outdoor nonresidential uses, hazards to flight, children's schools, hospitals, and nursing homes) within the project. However, as noted above, the proposed density is within the prohibited intermediate density range.

Noise: The property lies outside the area that would be subject to average exterior noise levels above 55 CNEL under ultimate airport development conditions. Therefore, no special noise attenuation measures are required for this residential land use.

Part 77: The elevation of Runway 8-26 at its westerly terminus is approximately 2,219 feet above mean sea level (AMSL). At a distance of approximately 4,930 feet from the runway, FAA review would be required for any structures with peak elevations exceeding 2,273.7 feet AMSL. The existing on-site elevations exceed 2,430 feet AMSL, and the Tentative Tract Map proposes a maximum pad elevation of 2,451.0 feet AMSL. Therefore, review of buildings by the FAA Obstruction Evaluation Services (FAAOES) was required. "Determination of No Hazard to Air Navigation" letters were issued by the FAAOES dated February 12, 2016, for Aeronautical Study Nos. 2015-AWP-8635-OE, 2015-AWP-8636-OE, 2015-AWP-8637-OE, 2015-AWP-8638-OE, and these studies revealed that the project's structures would exceed obstruction standards due primarily to site terrain, but that they would not be a hazard to air navigation because existing obstacles and terrain control the development of future approach and departure procedures at Banning Municipal Airport. Aircraft at normal Traffic Pattern altitudes and standard rates of descent would have reasonable clearance above these structures, even though the elevations would exceed the Airport's Conical Surface and Horizontal Surface. The letters include FAA OES conditions that have been incorporated into the project's conditions.

Open Area: Compatibility Zone D requires that 10% of area within major projects (10 acres or larger) be set aside as open area that could potentially serve as emergency landing areas. Based on the project's size of 10.67 acres, the project is required to provide a minimum of 1.067 acres of open area consistent with ALUC open area criteria. Lot B is identified as open space and is 1.33 acres in area; however, only 0.5 acre of that proposed lot is eligible for consideration as ALUC open area. (Portions of Lot B contain trees and other structures and obstructions which would not be permissible within ALUC open areas.) There is also a 30 foot wide Southern California Edison easement within Lot B which contains aboveground transmission lines that also would be considered as an obstruction. Therefore, Lot B's open area is insufficient to meet the Zone D criterion.

The Commission may consider, in addition to Lot B, Lot A as ALUC eligible open area, which is proposed for a detention basin/dog park. The dog park is 80 feet by 215 feet (which is inconsistent with ALUC minimum open area standards of 75 feet by 300 feet) and approximately 0.6 acres in area. There are no proposed structures besides chain link fencing located around the perimeter of the park. If the Commission chooses to accept this area in combination with Lot B, the project would have a total of 1.1 acres of open area and sufficient to meet Zone D criterion.

County Wide Policy 3.3.1 Infill: Countywide Policy 3.3.1 (Infill) is not useful in this situation, as it allows for greater densities than would otherwise be permitted in a Compatibility Zone, but does not provide for residential densities between 0.4 to 5.0 dwelling units per acre within Zone D. The proposed project would generally be compatible with the existing surrounding densities of single-family residential and mobile-home parks. The applicant has included a study of the existing surrounding single family residences within a 300 foot radius of the proposed project, resulting in an average density of 4.81 dwelling units per acre (du/ac). The map identifies several nearby existing tracts and densities: Hargrave Homes 1 4.36 du/ac, Luna Vista 3.72 du/ac, Monte Vista 4.14 du/ac, Virginia 3.97 du/ac, all of which lies within the prohibited intermediate density range of Zone D.

Countywide Policy 3.3.6 Other Special Conditions: While the project does not strictly comply with Zone D density criteria, the Commission may choose to consider whether to find the normally incompatible density and open area compatible pursuant to Countywide Policy 3.3.6 if the combination of the following facts are determined to represent “other extraordinary factors or circumstances” based on the following findings:

- The City of Banning has guidelines requiring a minimum lot width of fifty (50) feet, thus limiting the applicant’s ability to increase the project density within the single-family residential design model, which generally provides for lots that are 50-60 feet in width.

CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.

- (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, artificial marshes, wastewater management facilities, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Highly noise-sensitive outdoor nonresidential uses.
3. The attached notice shall be provided to all potential purchasers of the property, and shall be recorded as a deed notice.
 4. Any ground-level or aboveground water retention or detention basin or facilities shall be designed so as to provide for a detention period for the design storm that does not exceed 48 hours and to remain totally dry between rainfalls. Vegetation in and around such facilities that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature.
 5. The ALUC eligible open area provided in Lot B shall be kept obstacle and obstruction free per ALUC open area definition.
 6. The Federal Aviation Administration has conducted aeronautical studies of the proposed structures (Aeronautical Study Nos. 2015-AWP-8635-OE, 2015-AWP-8636-OE, 2015-AWP-8637-OE, and 2015-AWP-8638-OE), and has determined that neither marking nor lighting of the structure is necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 L and shall be maintained in accordance therewith for the life of the project.
 7. The maximum height of the proposed structures to top point shall not exceed 35 feet above ground level, and the maximum elevation at the top of the structures shall not exceed 2,486 feet above mean sea level.
 8. The specific coordinates, height and top point elevations of the proposed structures shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in structure height or elevation shall not require further review by the Airport Land Use Commission.

9. Temporary construction equipment used during actual construction of the structures shall not exceed the height of the structures, unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.
10. Within five (5) days after construction of the structures reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned or a decision is made not to construct the structures.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2015-AWP-8635-OE

Issued Date: 02/12/2016

Randall Andrus
RMG Residential 2010, LLLP
8800 N. Gainey Center Drive
Suite 255
Scottsdale, AZ 85258

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Building Single Family Homes
Location:	Banning, CA
Latitude:	33-56-03.54N NAD 83
Longitude:	116-52-23.16W
Heights:	2451 feet site elevation (SE) 35 feet above ground level (AGL) 2486 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure would have no substantial adverse effect on the safe and efficient utilization of the navigable airspace by aircraft or on the operation of air navigation facilities. Therefore, pursuant to the authority delegated to me, it is hereby determined that the structure would not be a hazard to air navigation provided the following condition(s) is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

At least 10 days prior to start of construction (7460-2, Part 1)
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed and maintained in accordance with FAA Advisory circular 70/7460-1 L.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

Any height exceeding 35 feet above ground level (2486 feet above mean sea level), will result in a substantial adverse effect and would warrant a Determination of Hazard to Air Navigation.

This determination expires on 08/12/2017 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is subject to review if an interested party files a petition that is received by the FAA on or before March 13, 2016. In the event a petition for review is filed, it must contain a full statement of the basis upon which it is made and be submitted to the Manager, Airspace Policy & Regulation, Federal Aviation Administration, 800 Independence Ave, SW, Room 423, Washington, DC 20591.

This determination becomes final on March 23, 2016 unless a petition is timely filed. In which case, this determination will not become final pending disposition of the petition. Interested parties will be notified of the grant of any review. For any questions regarding your petition, please contact Airspace Regulations & ATC Procedures Group via telephone -- 202-267-8783 - or facsimile 202-267-9328.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

This aeronautical study considered and analyzed the impact on existing and proposed arrival, departure, and en route procedures for aircraft operating under both visual flight rules and instrument flight rules; the impact on all existing and planned public-use airports, military airports and aeronautical facilities; and the cumulative impact resulting from the studied structure when combined with the impact of other existing or proposed

structures. The study disclosed that the described structure would have no substantial adverse effect on air navigation.

An account of the study findings, aeronautical objections received by the FAA during the study (if any), and the basis for the FAA's decision in this matter can be found on the following page(s).

If we can be of further assistance, please contact Karen McDonald, at (310) 725-6557. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2015-AWP-8635-OE.

Signature Control No: 261862242-281365398

(DNH)

Mike Helvey

Manager, Obstruction Evaluation Group

Attachment(s)

Additional Information

Map(s)

Additional information for ASN 2015-AWP-8635-OE

The proposal, submitted by RMC Residential 2020, LLLP, represents one boundary point for a potential development of 35-foot above ground level (agl) single family homes on significantly rising natural terrain in Banning, California. Viewed on a satellite map, this area of Banning has a concentration of existing structures, including utility poles, of similar height. This site point is located approximately 1.31 nautical miles (NM) northwest of the Banning Municipal (BNG) airport reference point. The BNG Field Elevation (FE) is 2222 feet above mean sea level (amsl). BNG is the closest civilian public-use landing area. The site elevation of this proposal site point is 2451 feet amsl.

The structure height exceeds the obstruction standards of Title 14 Code of Federal Regulations (CFR) Part 77 as follows:

Section 77.19(b) by 71 feet, the entire height of the structure above the ground, because of the significantly rising site terrain - a height exceeding the BNG Conical Surface.

Details of the proposal were not circularized for public aeronautical comment because internal FAA evaluation finds that the adverse effect of this structure is known. There would be no derogation of the navigable airspace overlying the site. Existing obstacles and terrain control the development of future approach and departure instrument Terminal Procedures at BNG. Therefore, no further attempt to negotiate the structure to a lower height was considered necessary. This does not affect the right to petition for review determinations regarding structures which exceed the subject obstruction standards.

AERONAUTICAL STUDY FOR POSSIBLE EFFECT UPON THE OPERATION OF AN AIR NAVIGATION AID:

- None.

AERONAUTICAL STUDY FOR POSSIBLE INSTRUMENT FLIGHT RULES (IFR) EFFECT DISCLOSED THE FOLLOWING:

- The proposal would have no effect on any existing or proposed IFR arrival/departure routes, operations, or procedures.
- The proposal would have no effect on any existing or proposed IFR en route routes, operations, or procedures.
- The proposal would have no effect on any existing or proposed IFR minimum flight altitudes.

AERONAUTICAL STUDY FOR POSSIBLE VISUAL FLIGHT RULES (VFR) EFFECT DISCLOSED THE FOLLOWING:

- The proposal would have no effect on any existing or proposed VFR arrival or departure routes, operations or procedures.
- The proposal would not conflict with airspace required to conduct normal VFR traffic pattern operations at any known public use or military airports, including BNG. The proposal would not penetrate a known maneuvering area associated with VFR Traffic Pattern operations at BNG than other existing structures of similar height seen on a satellite map. Aircraft at normal Traffic Pattern altitudes and standard rates of descent have reasonable clearance above this structure.

- The proposal would not penetrate those altitudes normally considered available to airmen for VFR en route flight.

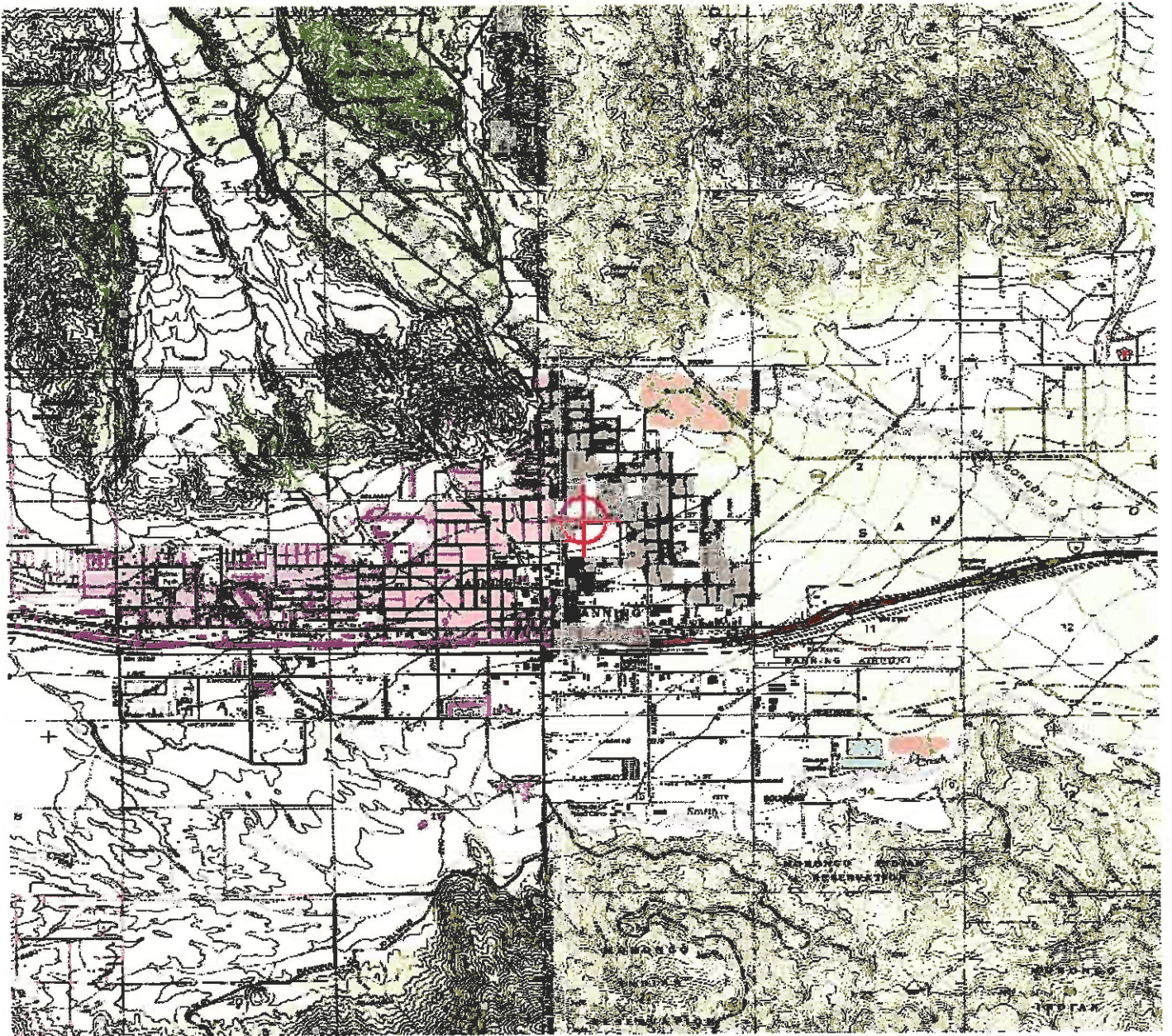
The cumulative impact of the proposed structure, when combined with other existing structures is not considered significant. Study did not disclose any adverse effect on existing or proposed public-use or military airports or navigational facilities. Nor would the proposal affect the capacity of any known existing or planned civilian public-use or military airport.

Therefore, it is determined that the proposed construction would not have a substantial adverse effect on the safe and efficient utilization of the navigable airspace by aircraft or on any air navigation facility and would not be a hazard to air navigation.

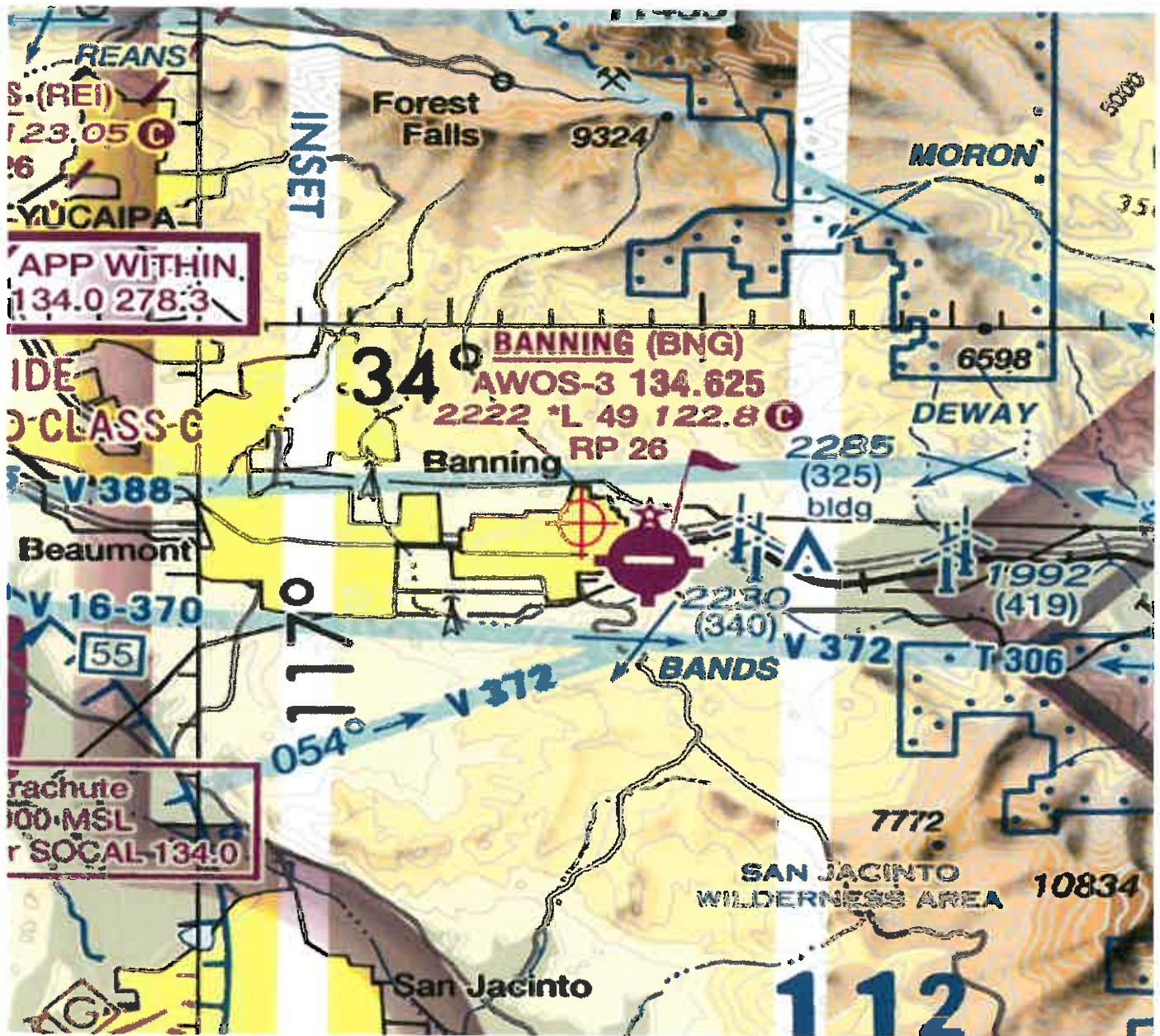
This determination, issued in accordance with Part 77, concerns the effect of the proposal on the safe and efficient use of the navigable airspace by aircraft and does not relieve the sponsor of any compliance responsibilities relating to laws, ordinances, or regulations of any Federal, state, or local governmental bodies.

Determinations, which are issued in accordance with Part 77, do not supersede or override any state, county, or local laws, aviation easements, or ordinances, or local zoning maximum heights.

TOPO Map for ASN 2015-AWP-8635-OE



Sectional Map for ASN 2015-AWP-8635-OE





Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2015-AWP-8636-OE

Issued Date: 02/12/2016

Randall Andrus
RMG Residential 2010, LLLP
8800 N. Gainey Center Drive
Suite 255
Scottsdale, AZ 85258

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Building Single Family Homes
Location:	Banning, CA
Latitude:	33-56-03.57N NAD 83
Longitude:	116-52-17.24W
Heights:	2445 feet site elevation (SE) 35 feet above ground level (AGL) 2480 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure would have no substantial adverse effect on the safe and efficient utilization of the navigable airspace by aircraft or on the operation of air navigation facilities. Therefore, pursuant to the authority delegated to me, it is hereby determined that the structure would not be a hazard to air navigation provided the following condition(s) is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

At least 10 days prior to start of construction (7460-2, Part 1)
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed and maintained in accordance with FAA Advisory circular 70/7460-1 L.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

Any height exceeding 35 feet above ground level (2480 feet above mean sea level), will result in a substantial adverse effect and would warrant a Determination of Hazard to Air Navigation.

This determination expires on 08/12/2017 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is subject to review if an interested party files a petition that is received by the FAA on or before March 13, 2016. In the event a petition for review is filed, it must contain a full statement of the basis upon which it is made and be submitted to the Manager, Airspace Policy & Regulation, Federal Aviation Administration, 800 Independence Ave, SW, Room 423, Washington, DC 20591.

This determination becomes final on March 23, 2016 unless a petition is timely filed. In which case, this determination will not become final pending disposition of the petition. Interested parties will be notified of the grant of any review. For any questions regarding your petition, please contact Airspace Regulations & ATC Procedures Group via telephone -- 202-267-8783 - or facsimile 202-267-9328.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

This aeronautical study considered and analyzed the impact on existing and proposed arrival, departure, and en route procedures for aircraft operating under both visual flight rules and instrument flight rules; the impact on all existing and planned public-use airports, military airports and aeronautical facilities; and the cumulative impact resulting from the studied structure when combined with the impact of other existing or proposed

structures. The study disclosed that the described structure would have no substantial adverse effect on air navigation.

An account of the study findings, aeronautical objections received by the FAA during the study (if any), and the basis for the FAA's decision in this matter can be found on the following page(s).

If we can be of further assistance, please contact Karen McDonald, at (310) 725-6557. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2015-AWP-8636-OE.

Signature Control No: 261862243-281365479

(DNH)

Mike Helvey

Manager, Obstruction Evaluation Group

Attachment(s)

Additional Information

Map(s)

Additional information for ASN 2015-AWP-8636-OE

The proposal, submitted by RMC Residential 2020, LLLP, represents one boundary point for a potential development of 35-foot above ground level (agl) single family homes on significantly rising natural terrain in Banning, California. Viewed on a satellite map, this area of Banning has a concentration of existing structures, including utility poles, of similar height. This site point is located approximately 1.24 nautical miles (NM) northwest of the Banning Municipal (BNG) airport reference point. The BNG Field Elevation (FE) is 2222 feet above mean sea level (amsl). BNG is the closest civilian public-use landing area. The site elevation of this proposal site point is 2445 feet amsl.

The structure height exceeds the obstruction standards of Title 14 Code of Federal Regulations (CFR) Part 77 as follows:

Section 77.19(b) by 82 feet, the entire height of the structure above the ground, because of the significantly rising site terrain - a height exceeding the BNG Conical Surface.

Details of the proposal were not circularized for public aeronautical comment because internal FAA evaluation finds that the adverse effect of this structure is known. There would be no derogation of the navigable airspace overlying the site. Existing obstacles and terrain control the development of future approach and departure instrument Terminal Procedures at BNG. Therefore, no further attempt to negotiate the structure to a lower height was considered necessary. This does not affect the right to petition for review determinations regarding structures which exceed the subject obstruction standards.

AERONAUTICAL STUDY FOR POSSIBLE EFFECT UPON THE OPERATION OF AN AIR NAVIGATION AID:

- None.

AERONAUTICAL STUDY FOR POSSIBLE INSTRUMENT FLIGHT RULES (IFR) EFFECT DISCLOSED THE FOLLOWING:

- The proposal would have no effect on any existing or proposed IFR arrival/departure routes, operations, or procedures.
- The proposal would have no effect on any existing or proposed IFR en route routes, operations, or procedures.
- The proposal would have no effect on any existing or proposed IFR minimum flight altitudes.

AERONAUTICAL STUDY FOR POSSIBLE VISUAL FLIGHT RULES (VFR) EFFECT DISCLOSED THE FOLLOWING:

- The proposal would have no effect on any existing or proposed VFR arrival or departure routes, operations or procedures.
- The proposal would not conflict with airspace required to conduct normal VFR traffic pattern operations at any known public use or military airports, including BNG. The proposal would not penetrate a known maneuvering area associated with VFR Traffic Pattern operations at BNG than other existing structures of similar height seen on a satellite map. Aircraft at normal Traffic Pattern altitudes and standard rates of descent have reasonable clearance above this structure.

- The proposal would not penetrate those altitudes normally considered available to airmen for VFR en route flight.

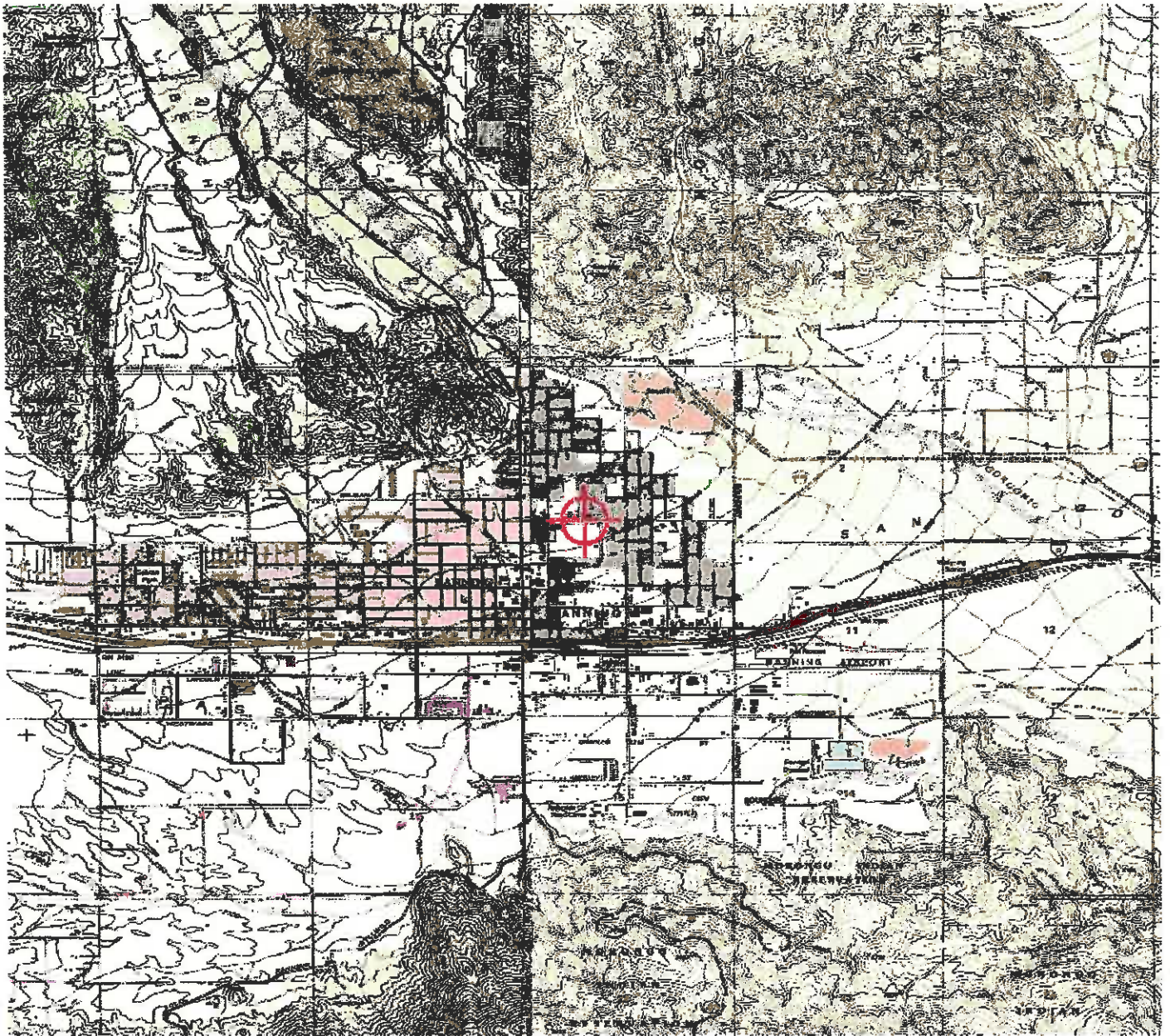
The cumulative impact of the proposed structure, when combined with other existing structures is not considered significant. Study did not disclose any adverse effect on existing or proposed public-use or military airports or navigational facilities. Nor would the proposal affect the capacity of any known existing or planned civilian public-use or military airport.

Therefore, it is determined that the proposed construction would not have a substantial adverse effect on the safe and efficient utilization of the navigable airspace by aircraft or on any air navigation facility and would not be a hazard to air navigation.

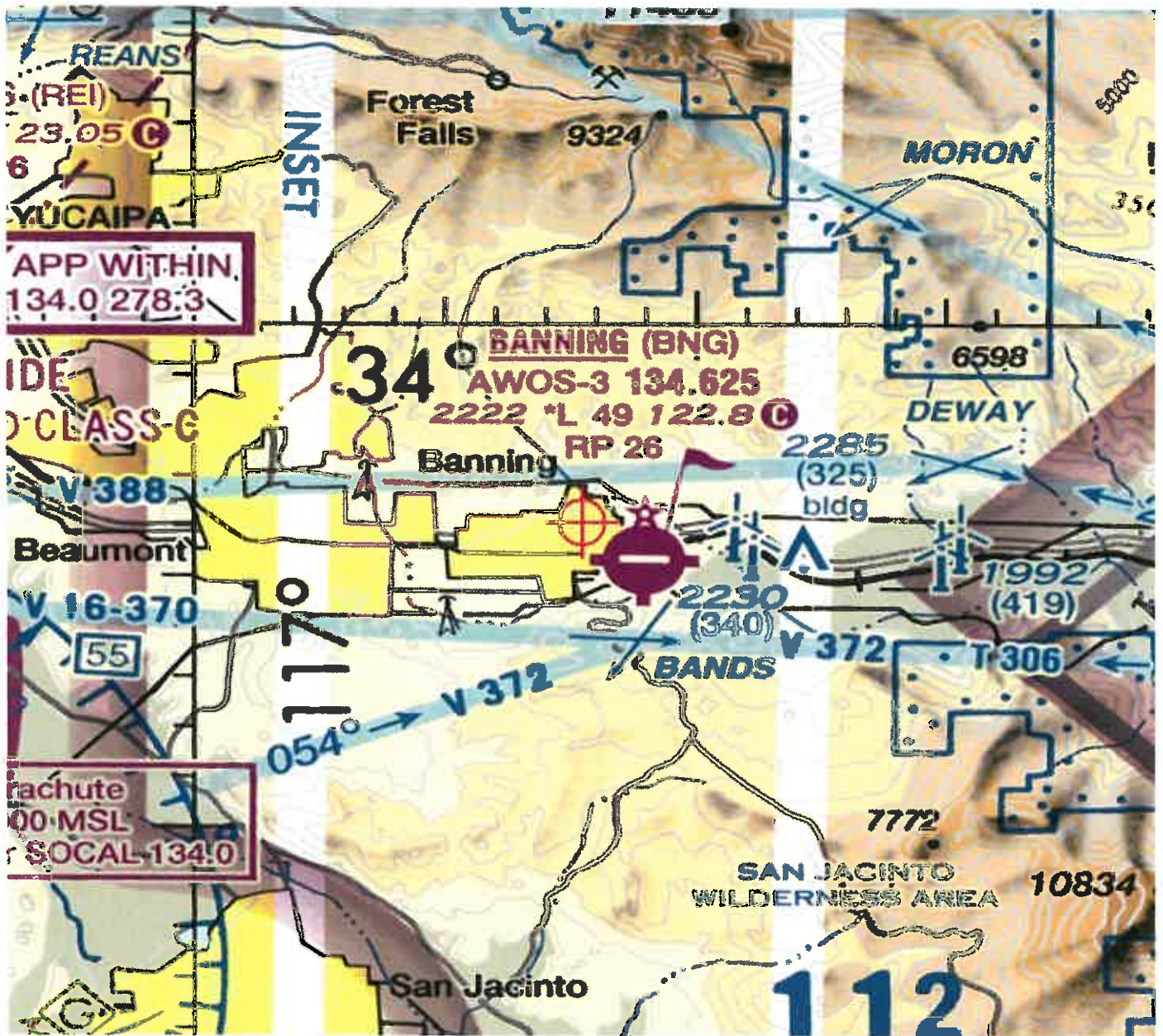
This determination, issued in accordance with Part 77, concerns the effect of the proposal on the safe and efficient use of the navigable airspace by aircraft and does not relieve the sponsor of any compliance responsibilities relating to laws, ordinances, or regulations of any Federal, state, or local governmental bodies.

Determinations, which are issued in accordance with Part 77, do not supersede or override any state, county, or local laws, aviation easements, or ordinances, or local zoning maximum heights.

TOPO Map for ASN 2015-AWP-8636-OE



Sectional Map for ASN 2015-AWP-8636-OE





Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2015-AWP-8637-OE

Issued Date: 02/12/2016

Randall Andrus
RMG Residential 2010, LLLP
8800 N. Gainey Center Drive
Suite 255
Scottsdale, AZ 85258

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Building Single Family Homes
Location:	Banning, CA
Latitude:	33-55-57.58N NAD 83
Longitude:	116-52-25.06W
Heights:	2432 feet site elevation (SE) 35 feet above ground level (AGL) 2467 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure would have no substantial adverse effect on the safe and efficient utilization of the navigable airspace by aircraft or on the operation of air navigation facilities. Therefore, pursuant to the authority delegated to me, it is hereby determined that the structure would not be a hazard to air navigation provided the following condition(s) is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed and maintained in accordance with FAA Advisory circular 70/7460-1 L.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

Any height exceeding 35 feet above ground level (2467 feet above mean sea level), will result in a substantial adverse effect and would warrant a Determination of Hazard to Air Navigation.

This determination expires on 08/12/2017 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is subject to review if an interested party files a petition that is received by the FAA on or before March 13, 2016. In the event a petition for review is filed, it must contain a full statement of the basis upon which it is made and be submitted to the Manager, Airspace Policy & Regulation, Federal Aviation Administration, 800 Independence Ave, SW, Room 423, Washington, DC 20591.

This determination becomes final on March 23, 2016 unless a petition is timely filed. In which case, this determination will not become final pending disposition of the petition. Interested parties will be notified of the grant of any review. For any questions regarding your petition, please contact Airspace Regulations & ATC Procedures Group via telephone -- 202-267-8783 - or facsimile 202-267-9328.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

This aeronautical study considered and analyzed the impact on existing and proposed arrival, departure, and en route procedures for aircraft operating under both visual flight rules and instrument flight rules; the impact on all existing and planned public-use airports, military airports and aeronautical facilities; and the cumulative impact resulting from the studied structure when combined with the impact of other existing or proposed

structures. The study disclosed that the described structure would have no substantial adverse effect on air navigation.

An account of the study findings, aeronautical objections received by the FAA during the study (if any), and the basis for the FAA's decision in this matter can be found on the following page(s).

If we can be of further assistance, please contact Karen McDonald, at (310) 725-6557. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2015-AWP-8637-OE.

Signature Control No: 261862244-281365581

(DNH)

Mike Helvey

Manager, Obstruction Evaluation Group

Attachment(s)

Additional Information

Map(s)

Additional information for ASN 2015-AWP-8637-OE

The proposal, submitted by RMC Residential 2020, LLLP, represents one boundary point for a potential development of 35-foot above ground level (agl) single family homes on significantly rising natural terrain in Banning, California. Viewed on a satellite map, this area of Banning has a concentration of existing structures, including utility poles, of similar height. This site point is located approximately 1.28 nautical miles (NM) northwest of the Banning Municipal (BNG) airport reference point. The BNG Field Elevation (FE) is 2222 feet above mean sea level (amsl). BNG is the closest civilian public-use landing area. The site elevation of this proposal site point is 2432 feet amsl.

The structure height exceeds the obstruction standards of Title 14 Code of Federal Regulations (CFR) Part 77 as follows:

Section 77.19(b) by 67 feet, the entire height of the structure above the ground, because of the significantly rising site terrain - a height exceeding the BNG Conical Surface.

Details of the proposal were not circularized for public aeronautical comment because internal FAA evaluation finds that the adverse effect of this structure is known. There would be no derogation of the navigable airspace overlying the site. Existing obstacles and terrain control the development of future approach and departure instrument Terminal Procedures at BNG. Therefore, no further attempt to negotiate the structure to a lower height was considered necessary. This does not affect the right to petition for review determinations regarding structures which exceed the subject obstruction standards.

AERONAUTICAL STUDY FOR POSSIBLE EFFECT UPON THE OPERATION OF AN AIR NAVIGATION AID:

- None.

AERONAUTICAL STUDY FOR POSSIBLE INSTRUMENT FLIGHT RULES (IFR) EFFECT DISCLOSED THE FOLLOWING:

- The proposal would have no effect on any existing or proposed IFR arrival/departure routes, operations, or procedures.
- The proposal would have no effect on any existing or proposed IFR en route routes, operations, or procedures.
- The proposal would have no effect on any existing or proposed IFR minimum flight altitudes.

AERONAUTICAL STUDY FOR POSSIBLE VISUAL FLIGHT RULES (VFR) EFFECT DISCLOSED THE FOLLOWING:

- The proposal would have no effect on any existing or proposed VFR arrival or departure routes, operations or procedures.
- The proposal would not conflict with airspace required to conduct normal VFR traffic pattern operations at any known public use or military airports, including BNG. The proposal would not penetrate a known maneuvering area associated with VFR Traffic Pattern operations at BNG than other existing structures of similar height seen on a satellite map. Aircraft at normal Traffic Pattern altitudes and standard rates of descent have reasonable clearance above this structure.

- The proposal would not penetrate those altitudes normally considered available to airmen for VFR en route flight.

The cumulative impact of the proposed structure, when combined with other existing structures is not considered significant. Study did not disclose any adverse effect on existing or proposed public-use or military airports or navigational facilities. Nor would the proposal affect the capacity of any known existing or planned civilian public-use or military airport.

Therefore, it is determined that the proposed construction would not have a substantial adverse effect on the safe and efficient utilization of the navigable airspace by aircraft or on any air navigation facility and would not be a hazard to air navigation.

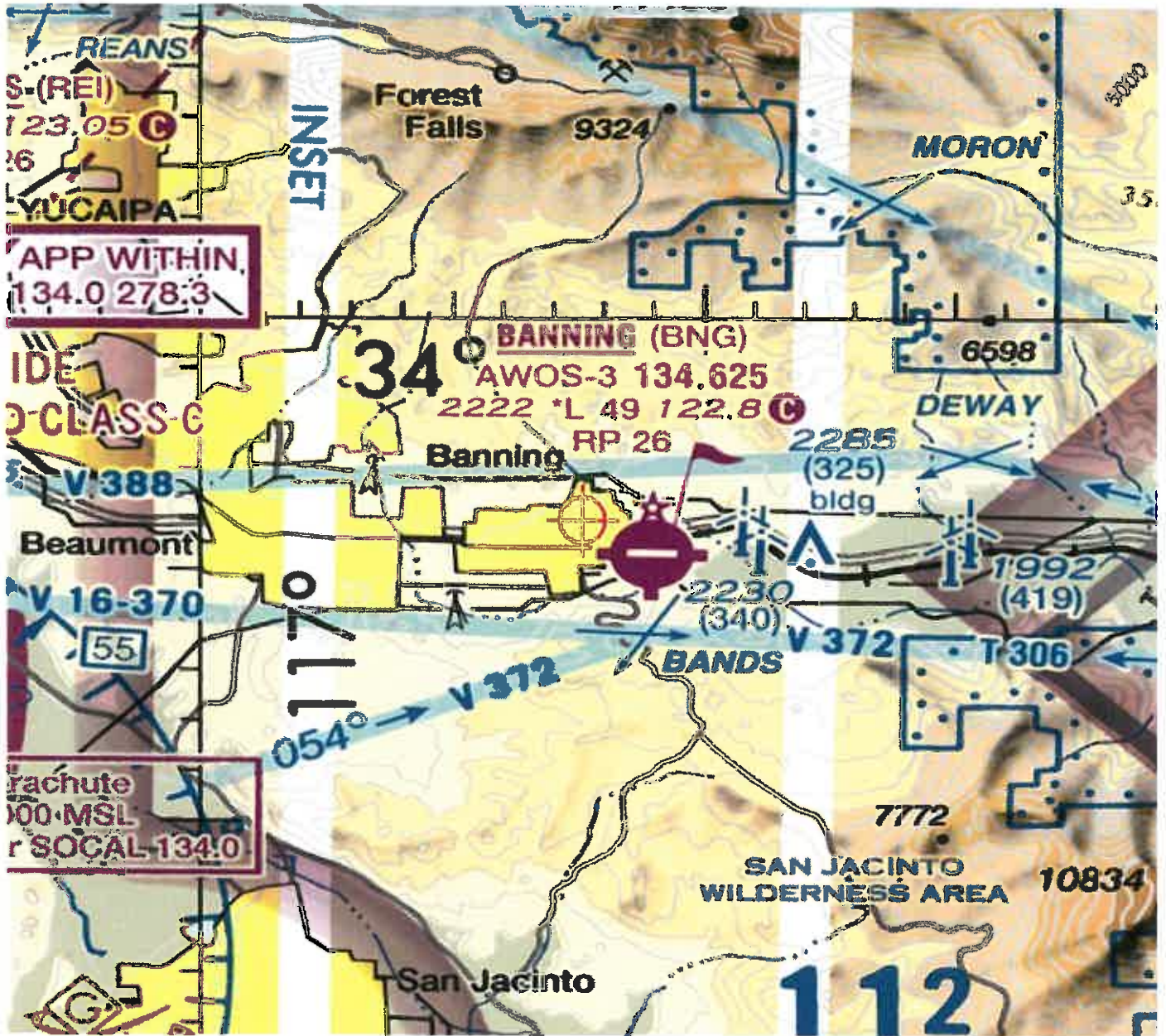
This determination, issued in accordance with Part 77, concerns the effect of the proposal on the safe and efficient use of the navigable airspace by aircraft and does not relieve the sponsor of any compliance responsibilities relating to laws, ordinances, or regulations of any Federal, state, or local governmental bodies.

Determinations, which are issued in accordance with Part 77, do not supersede or override any state, county, or local laws, aviation easements, or ordinances, or local zoning maximum heights.

TOPO Map for ASN 2015-AWP-8637-OE



Sectional Map for ASN 2015-AWP-8637-OE





Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2015-AWP-8638-OE

Issued Date: 02/12/2016

Randall Andrus
RMG Residential 2010, LLLP
8800 N. Gainey Center Drive
Suite 255
Scottsdale, AZ 85258

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building Single Family Homes
Location: Banning, CA
Latitude: 33-55-57.69N NAD 83
Longitude: 116-52-13.08W
Heights: 2426 feet site elevation (SE)
35 feet above ground level (AGL)
2461 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure would have no substantial adverse effect on the safe and efficient utilization of the navigable airspace by aircraft or on the operation of air navigation facilities. Therefore, pursuant to the authority delegated to me, it is hereby determined that the structure would not be a hazard to air navigation provided the following condition(s) is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

At least 10 days prior to start of construction (7460-2, Part 1)
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed and maintained in accordance with FAA Advisory circular 70/7460-1 L.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

Any height exceeding 35 feet above ground level (2461 feet above mean sea level), will result in a substantial adverse effect and would warrant a Determination of Hazard to Air Navigation.

This determination expires on 08/12/2017 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is subject to review if an interested party files a petition that is received by the FAA on or before March 13, 2016. In the event a petition for review is filed, it must contain a full statement of the basis upon which it is made and be submitted to the Manager, Airspace Policy & Regulation, Federal Aviation Administration, 800 Independence Ave, SW, Room 423, Washington, DC 20591.

This determination becomes final on March 23, 2016 unless a petition is timely filed. In which case, this determination will not become final pending disposition of the petition. Interested parties will be notified of the grant of any review. For any questions regarding your petition, please contact Airspace Regulations & ATC Procedures Group via telephone -- 202-267-8783 - or facsimile 202-267-9328.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

This aeronautical study considered and analyzed the impact on existing and proposed arrival, departure, and en route procedures for aircraft operating under both visual flight rules and instrument flight rules; the impact on all existing and planned public-use airports, military airports and aeronautical facilities; and the cumulative impact resulting from the studied structure when combined with the impact of other existing or proposed

structures. The study disclosed that the described structure would have no substantial adverse effect on air navigation.

An account of the study findings, aeronautical objections received by the FAA during the study (if any), and the basis for the FAA's decision in this matter can be found on the following page(s).

If we can be of further assistance, please contact Karen McDonald, at (310) 725-6557. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2015-AWP-8638-OE.

Signature Control No: 261862245-281365616

(DNH)

Mike Helvey

Manager, Obstruction Evaluation Group

Attachment(s)

Additional Information

Map(s)

Additional information for ASN 2015-AWP-8638-OE

The proposal, submitted by RMC Residential 2020, LLLP, represents one boundary point for a potential development of 35-foot above ground level (agl) single family homes on significantly rising natural terrain in Banning, California. Viewed on a satellite map, this area of Banning has a concentration of existing structures, including utility poles, of similar height. This site point is located approximately 1.14 nautical miles (NM) northwest of the Banning Municipal (BNG) airport reference point. The BNG Field Elevation (FE) is 2222 feet above mean sea level (amsl). BNG is the closest civilian public-use landing area. The site elevation of this proposal site point is 2426 feet amsl.

The structure height exceeds the obstruction standards of Title 14 Code of Federal Regulations (CFR) Part 77 as follows:

Section 77.19(a) by 89 feet, the entire height of the structure above the ground, because of the significantly rising site terrain - a height exceeding the BNG Horizontal Surface.

Details of the proposal were not circularized for public aeronautical comment because internal FAA evaluation finds that the adverse effect of this structure is known. There would be no derogation of the navigable airspace overlying the site. Existing obstacles and terrain control the development of future approach and departure instrument Terminal Procedures at BNG. Therefore, no further attempt to negotiate the structure to a lower height was considered necessary. This does not affect the right to petition for review determinations regarding structures which exceed the subject obstruction standards.

AERONAUTICAL STUDY FOR POSSIBLE EFFECT UPON THE OPERATION OF AN AIR NAVIGATION AID:

- None.

AERONAUTICAL STUDY FOR POSSIBLE INSTRUMENT FLIGHT RULES (IFR) EFFECT DISCLOSED THE FOLLOWING:

- The proposal would have no effect on any existing or proposed IFR arrival/departure routes, operations, or procedures.
- The proposal would have no effect on any existing or proposed IFR en route routes, operations, or procedures.
- The proposal would have no effect on any existing or proposed IFR minimum flight altitudes.

AERONAUTICAL STUDY FOR POSSIBLE VISUAL FLIGHT RULES (VFR) EFFECT DISCLOSED THE FOLLOWING:

- The proposal would have no effect on any existing or proposed VFR arrival or departure routes, operations or procedures.
- The proposal would not conflict with airspace required to conduct normal VFR traffic pattern operations at any known public use or military airports, including BNG. The proposal would not penetrate a known maneuvering area associated with VFR Traffic Pattern operations at BNG than other existing structures of similar height seen on a satellite map. Aircraft at normal Traffic Pattern altitudes and standard rates of descent have reasonable clearance above this structure.

- The proposal would not penetrate those altitudes normally considered available to airmen for VFR en route flight.

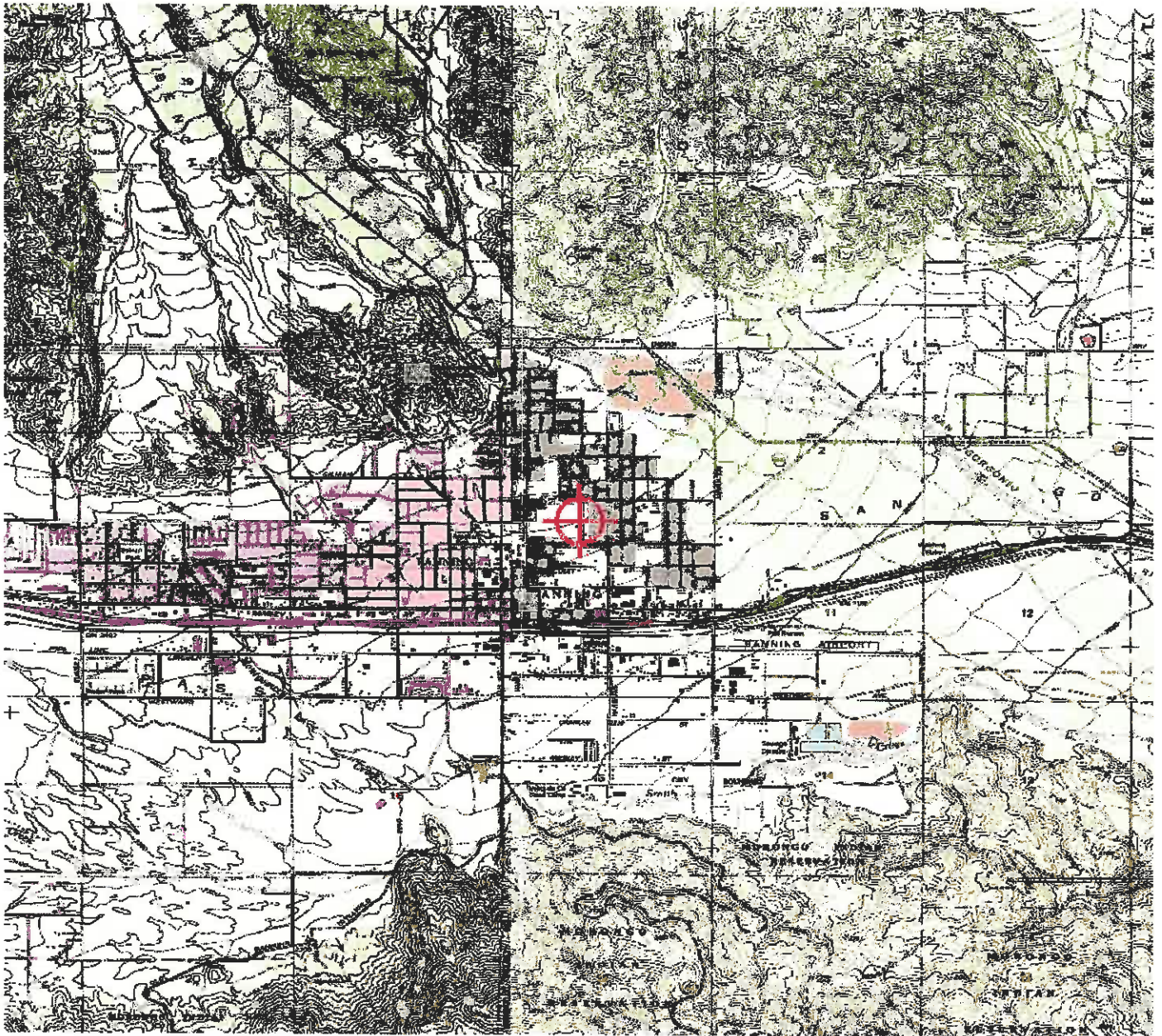
The cumulative impact of the proposed structure, when combined with other existing structures is not considered significant. Study did not disclose any adverse effect on existing or proposed public-use or military airports or navigational facilities. Nor would the proposal affect the capacity of any known existing or planned civilian public-use or military airport.

Therefore, it is determined that the proposed construction would not have a substantial adverse effect on the safe and efficient utilization of the navigable airspace by aircraft or on any air navigation facility and would not be a hazard to air navigation.

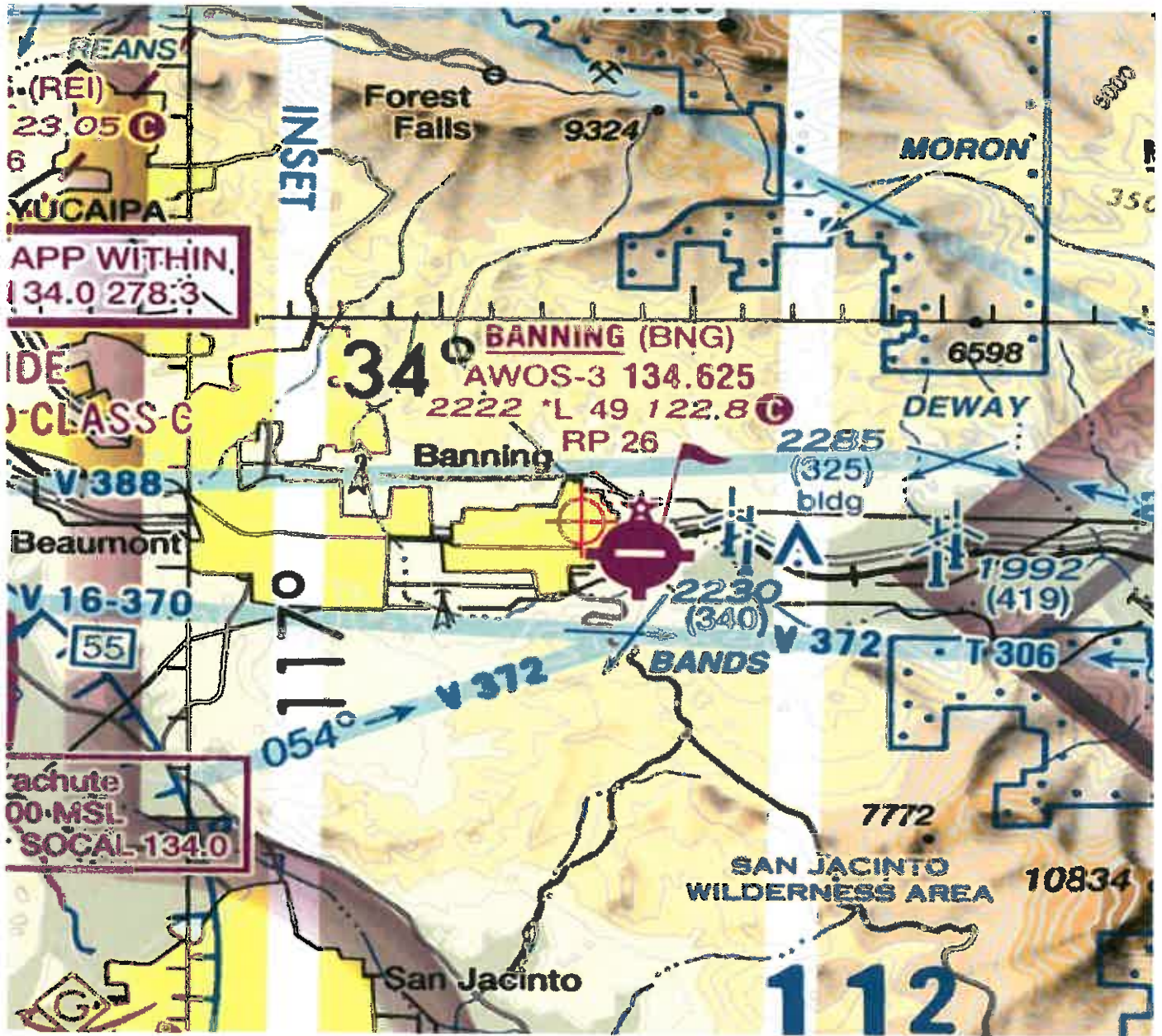
This determination, issued in accordance with Part 77, concerns the effect of the proposal on the safe and efficient use of the navigable airspace by aircraft and does not relieve the sponsor of any compliance responsibilities relating to laws, ordinances, or regulations of any Federal, state, or local governmental bodies.

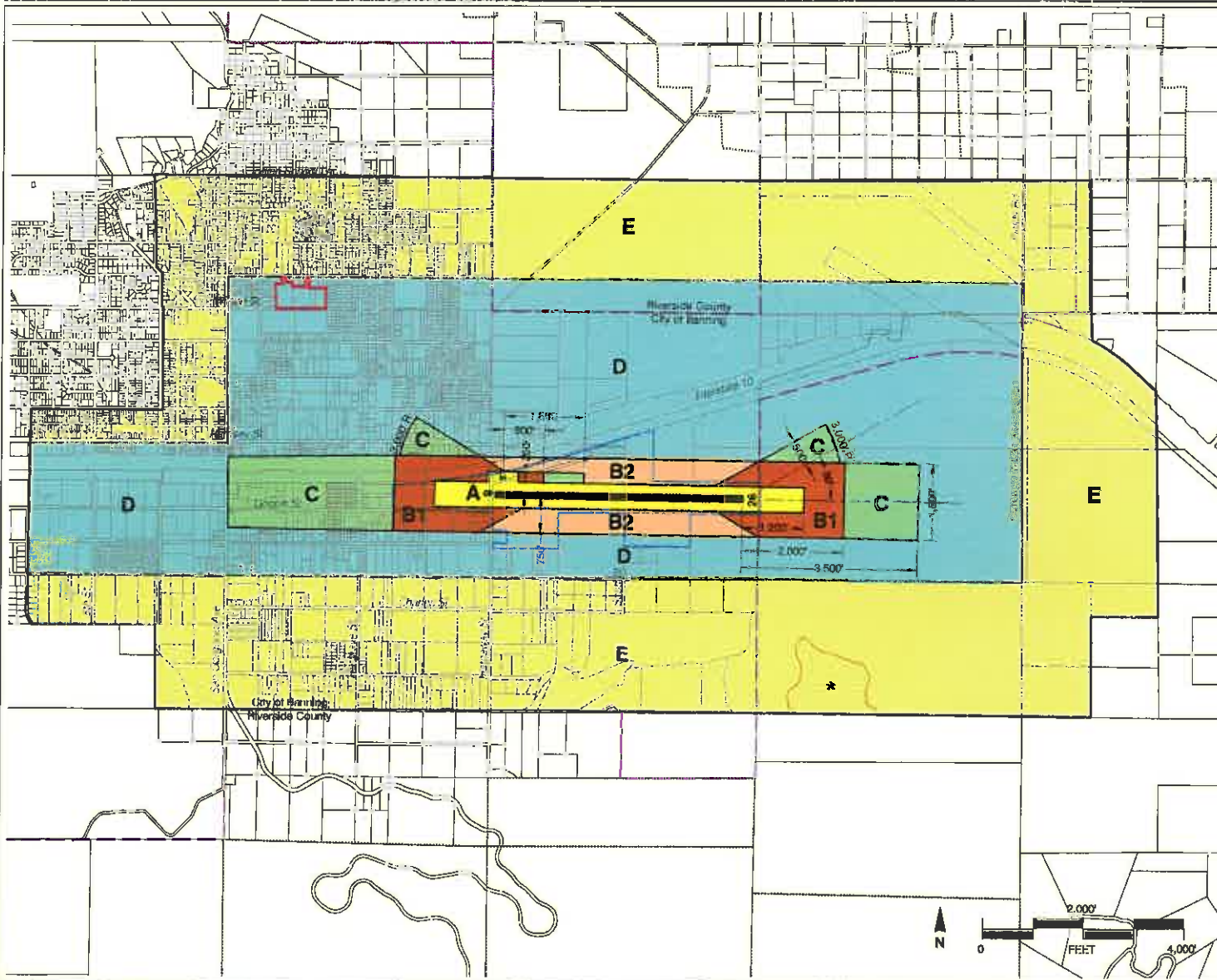
Determinations, which are issued in accordance with Part 77, do not supersede or override any state, county, or local laws, aviation easements, or ordinances, or local zoning maximum heights.

TOPO Map for ASN 2015-AWP-8638-OE



Sectional Map for ASN 2015-AWP-8638-OE





Legend

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E
- Height Review Overlay Zone

Boundary Lines

- Airport Property Line
- City Limits
- Morongo Indian Reservation

Note

Dimensions measured from runway ends and centerlines.
See Chapter 2, Table 2A for compatibility criteria associated with this map.

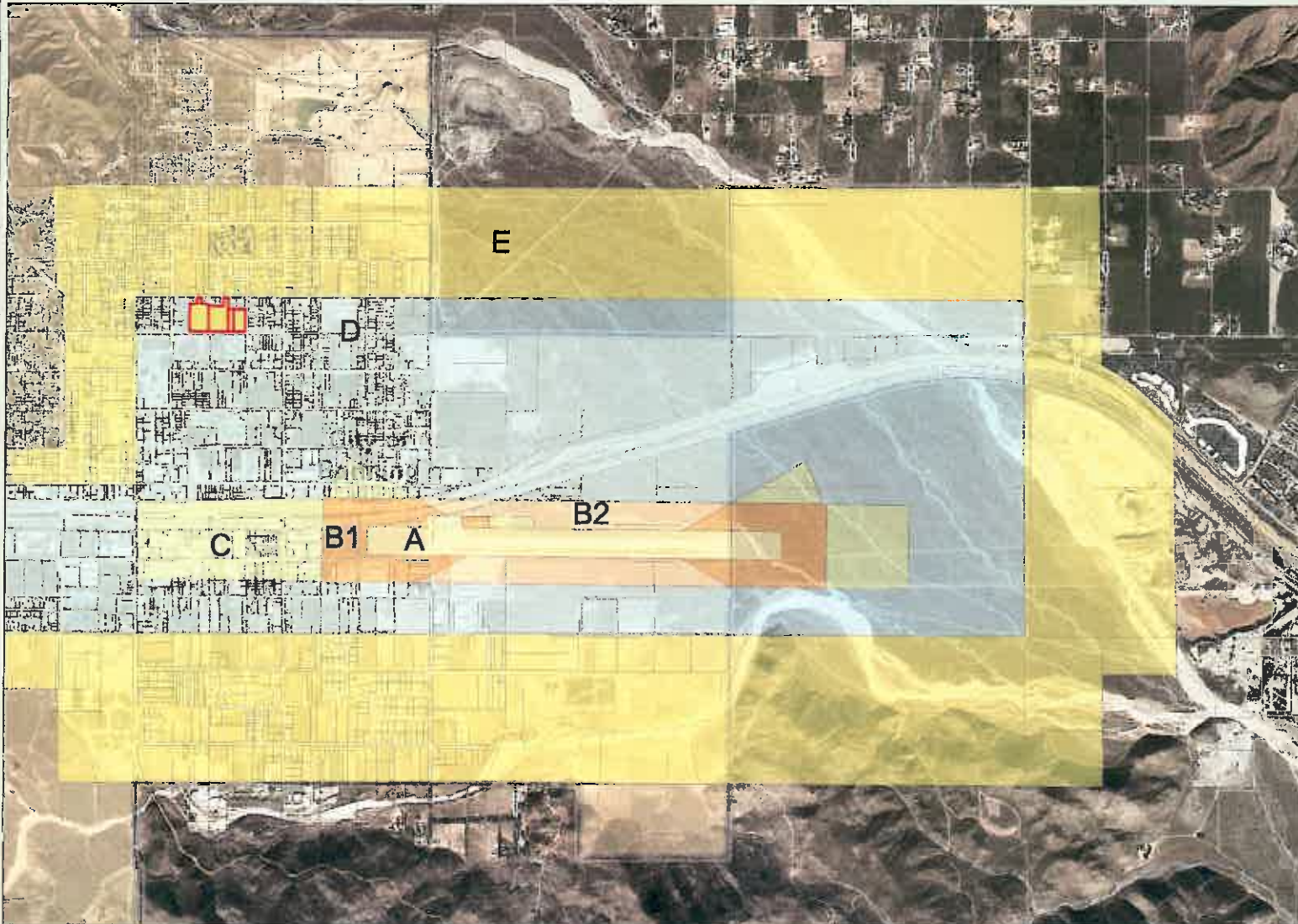
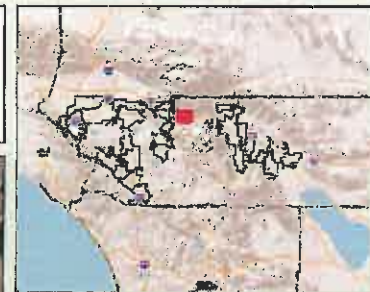
Riverside County
Airport Land Use Commission
**Riverside County
Airport Land Use Compatibility Plan
Policy Document**
(Adopted October 2004)

Map 15N-1

Compatibility Map
Banning Municipal Airport



My Map



Legend

Airport Compatibility

- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT
- C2-HIGHT-EXC1
- C2-HIGHT-EXC5



0 3,325 6,649 Feet



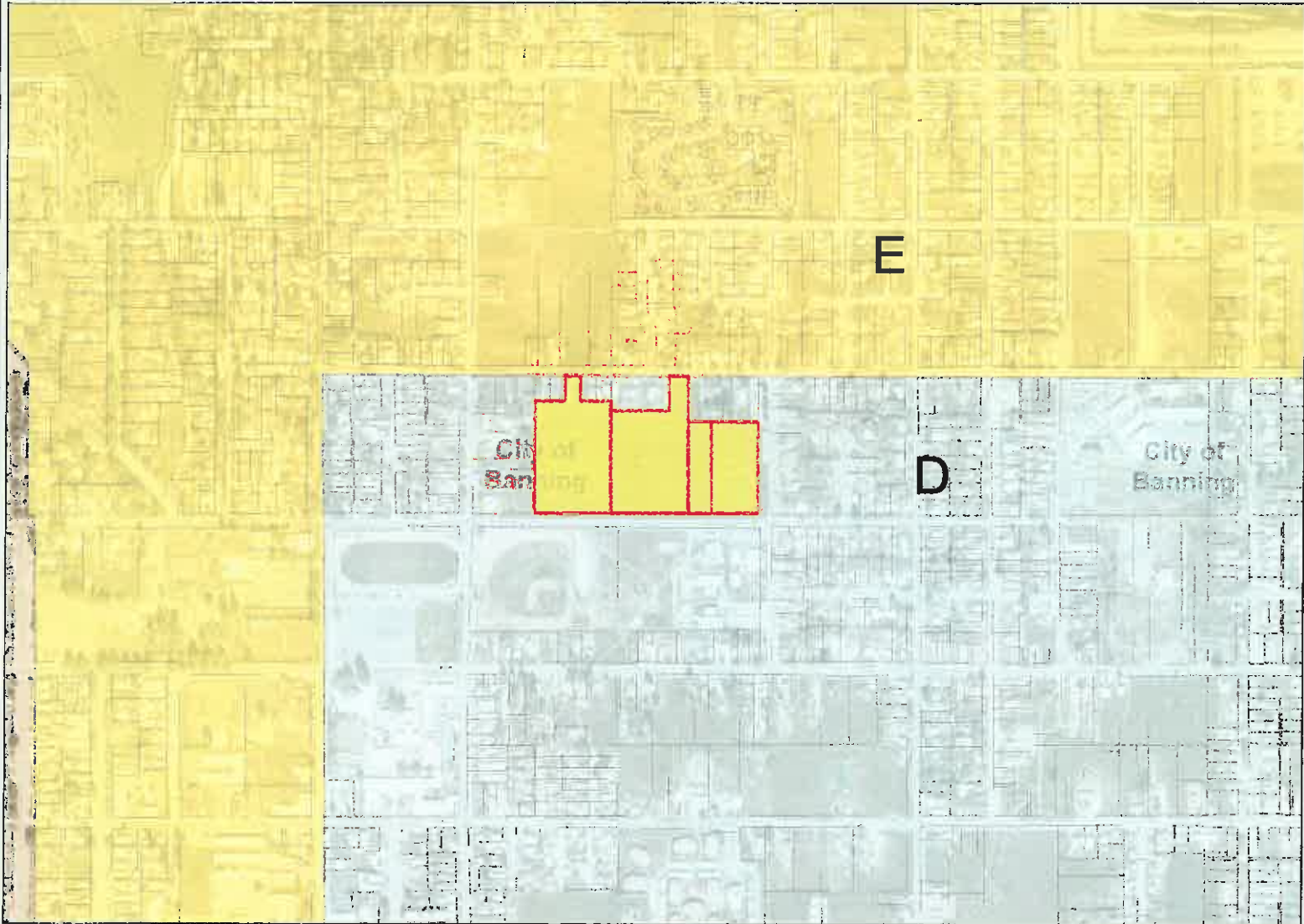
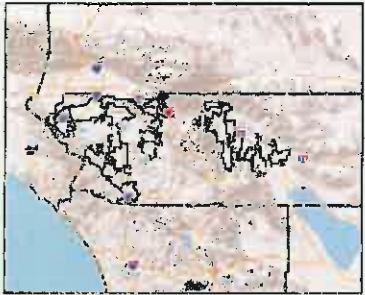
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/13/2015 10:43:13 AM

© Riverside County TLMA GIS

Notes

My Map



Legend

- Display Parcels
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT
- C2-HIGHT-EXC1



0 831 1,662 Feet



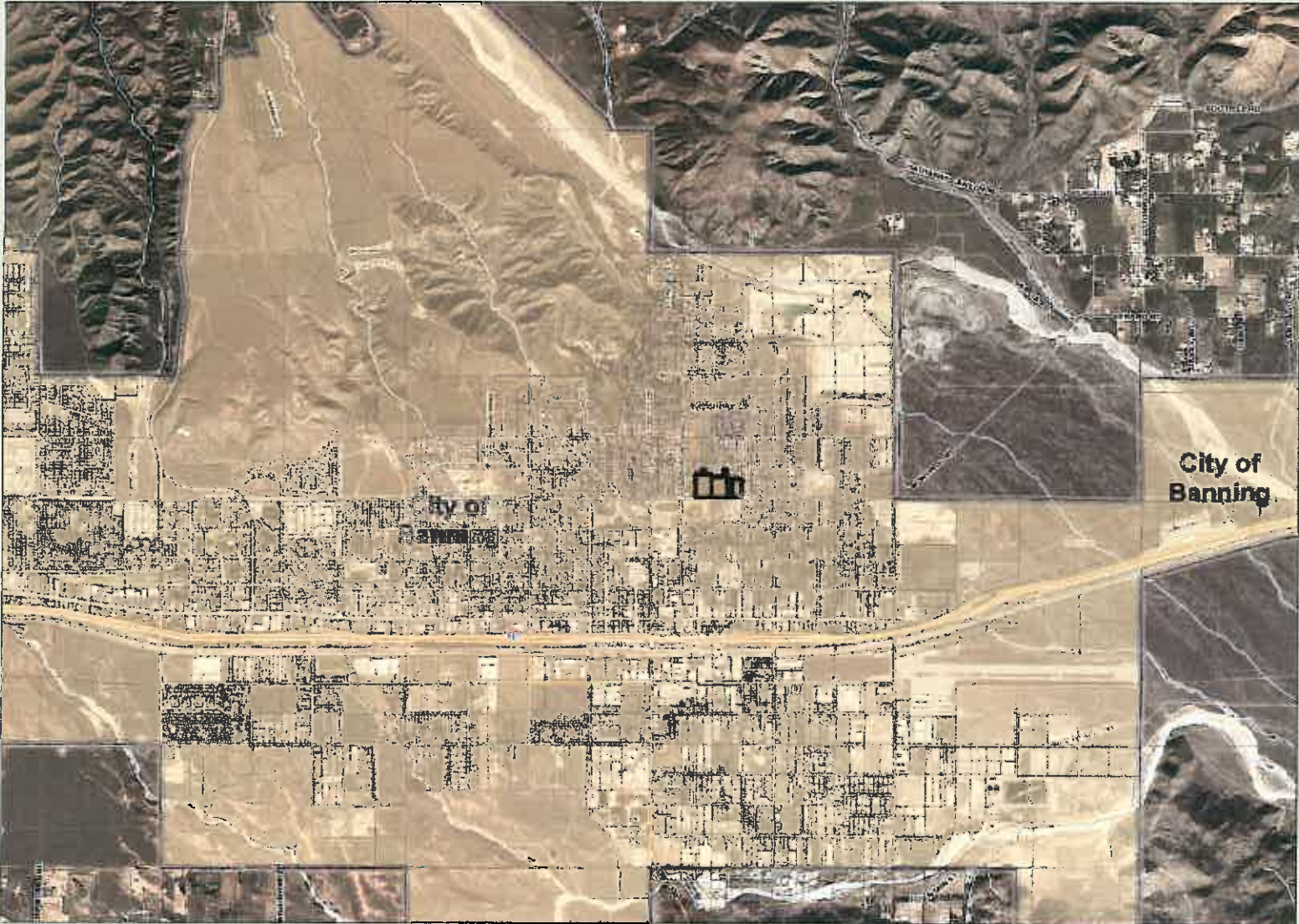
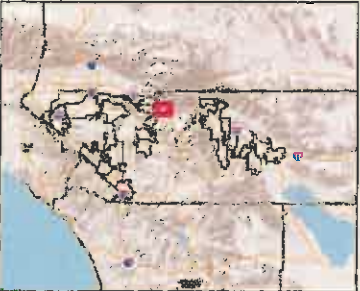
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/13/2015 10:42:37 AM

© Riverside County TLMA GIS

Notes

My Map



Legend

- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 4,027 8,054 Feet



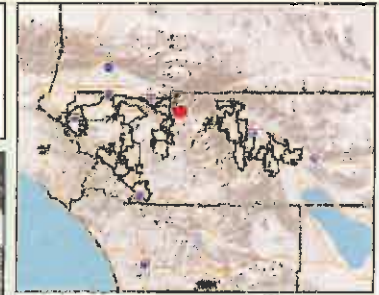
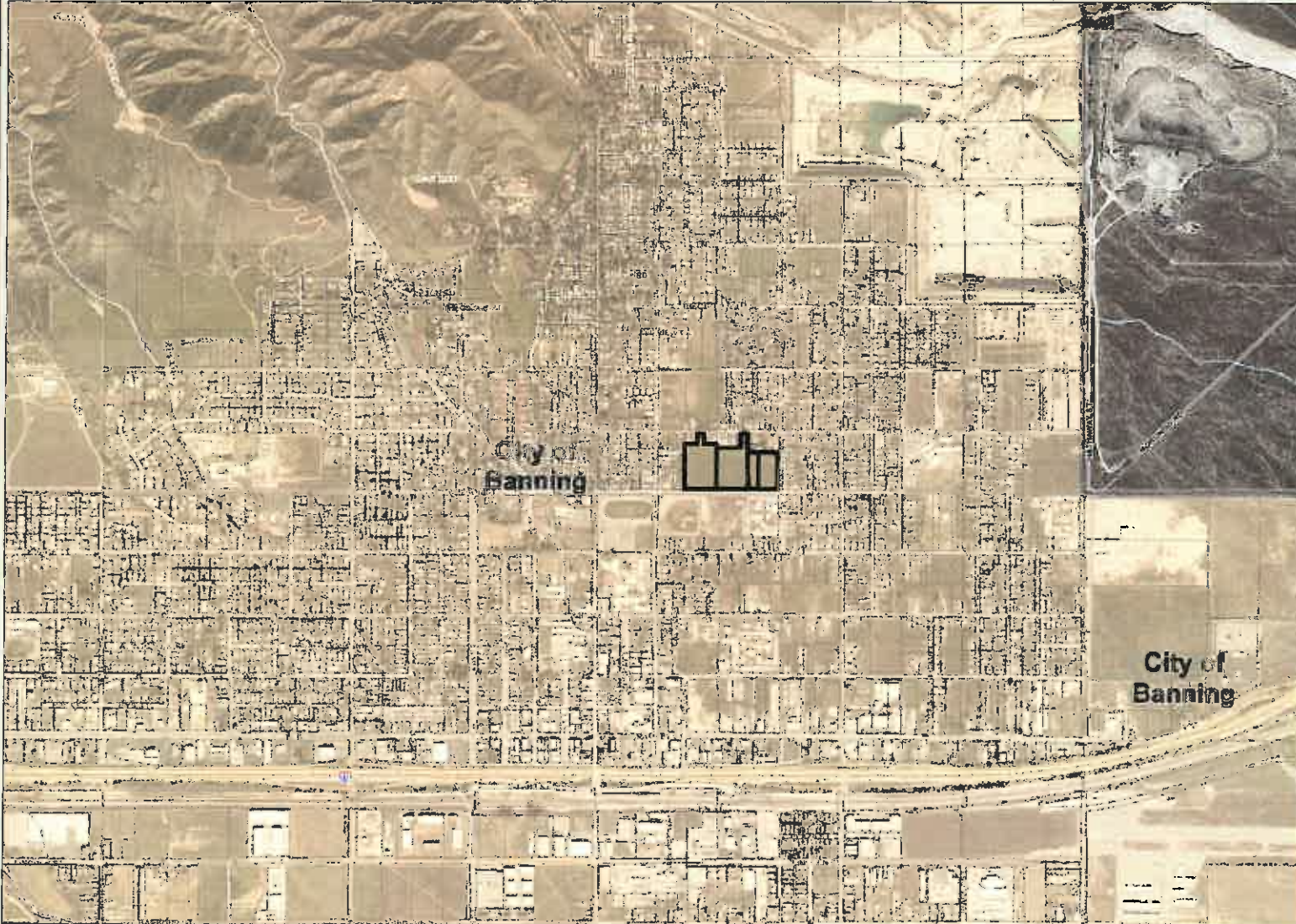
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 9/9/2016 2:31:41 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



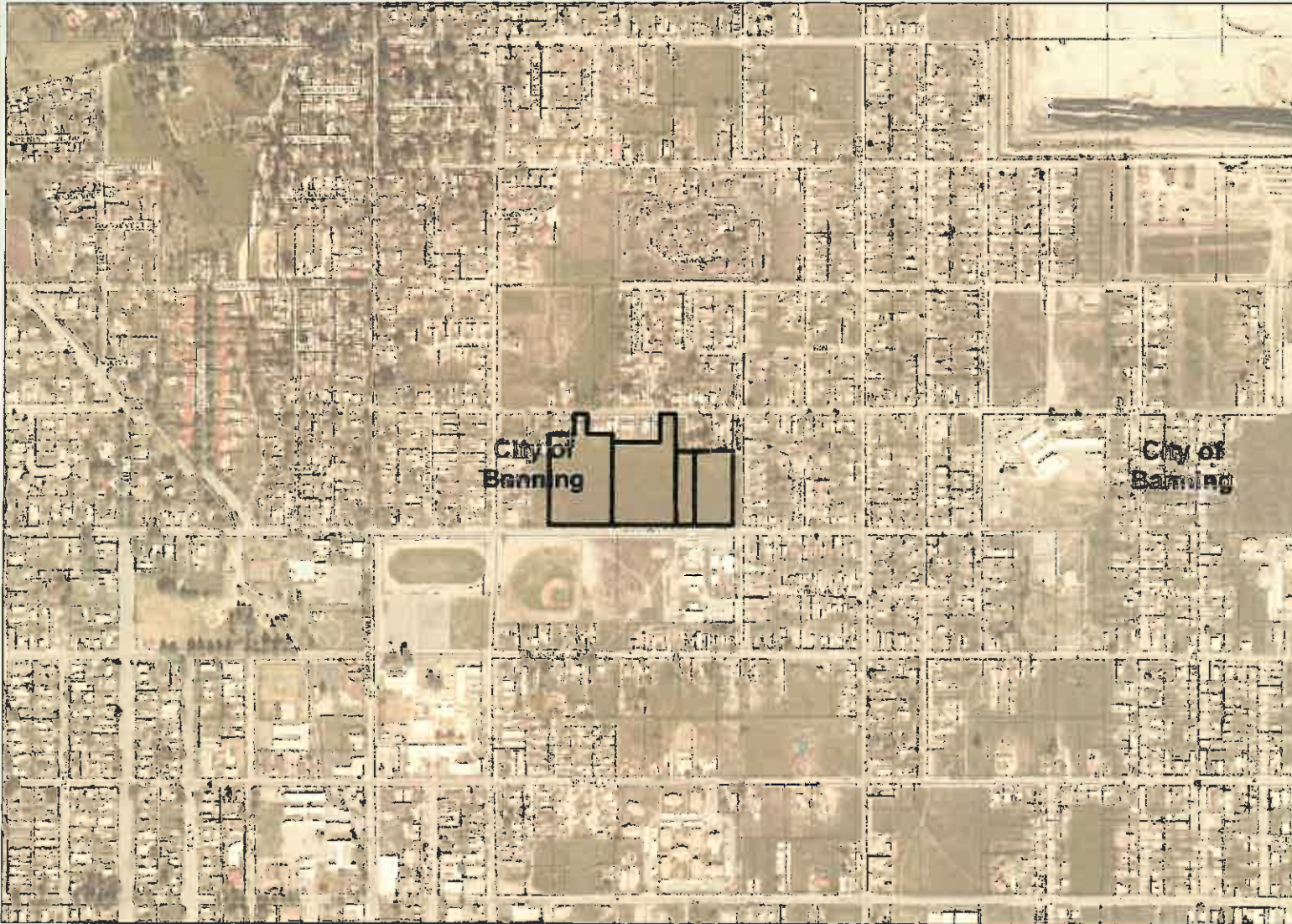
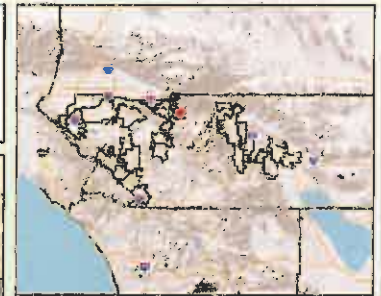
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 9/9/2016 2:32:09 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 1,007 2,014 Feet



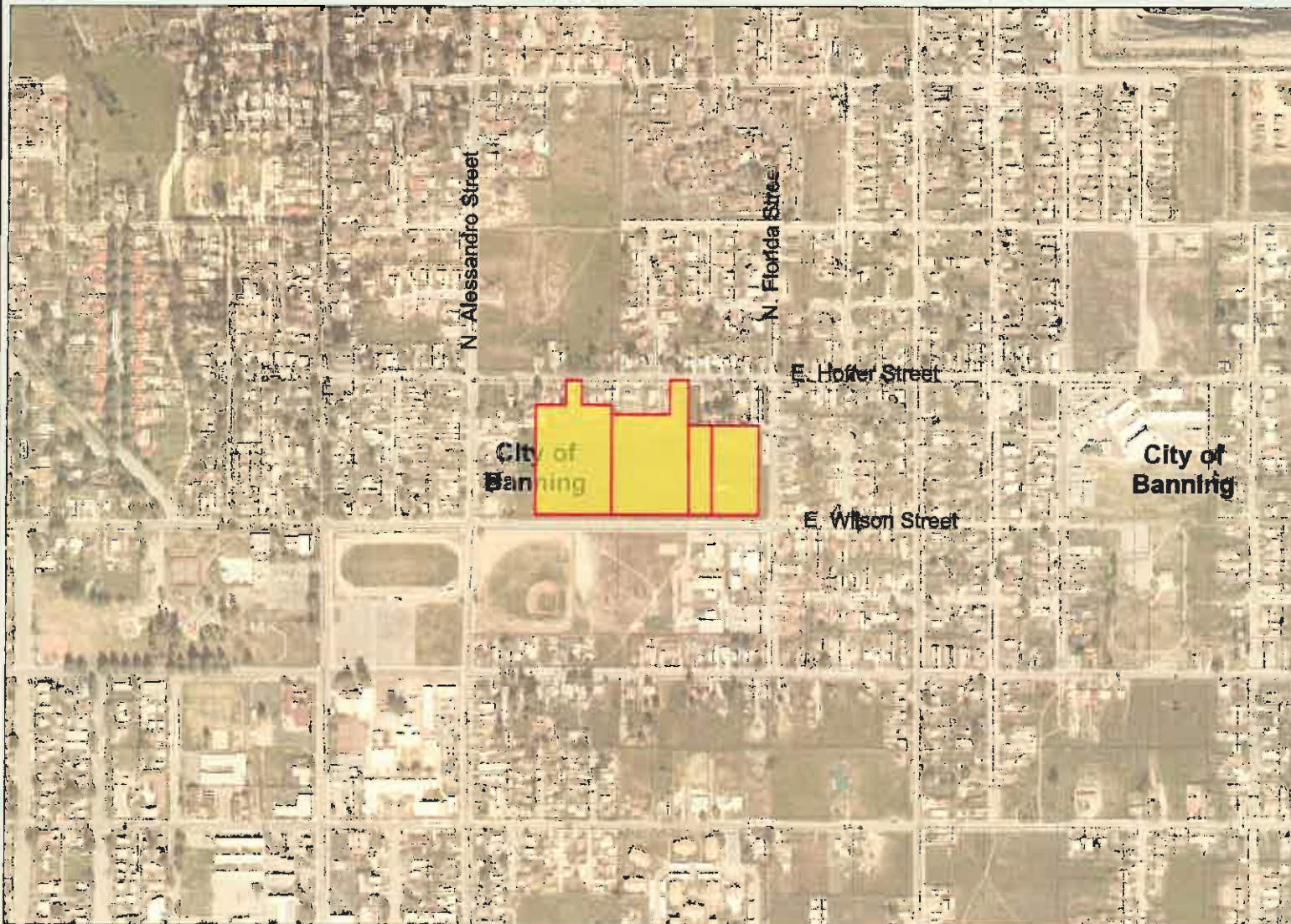
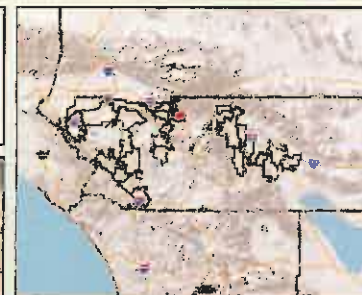
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 9/9/2016 2:33:01 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 831 1,662 Feet



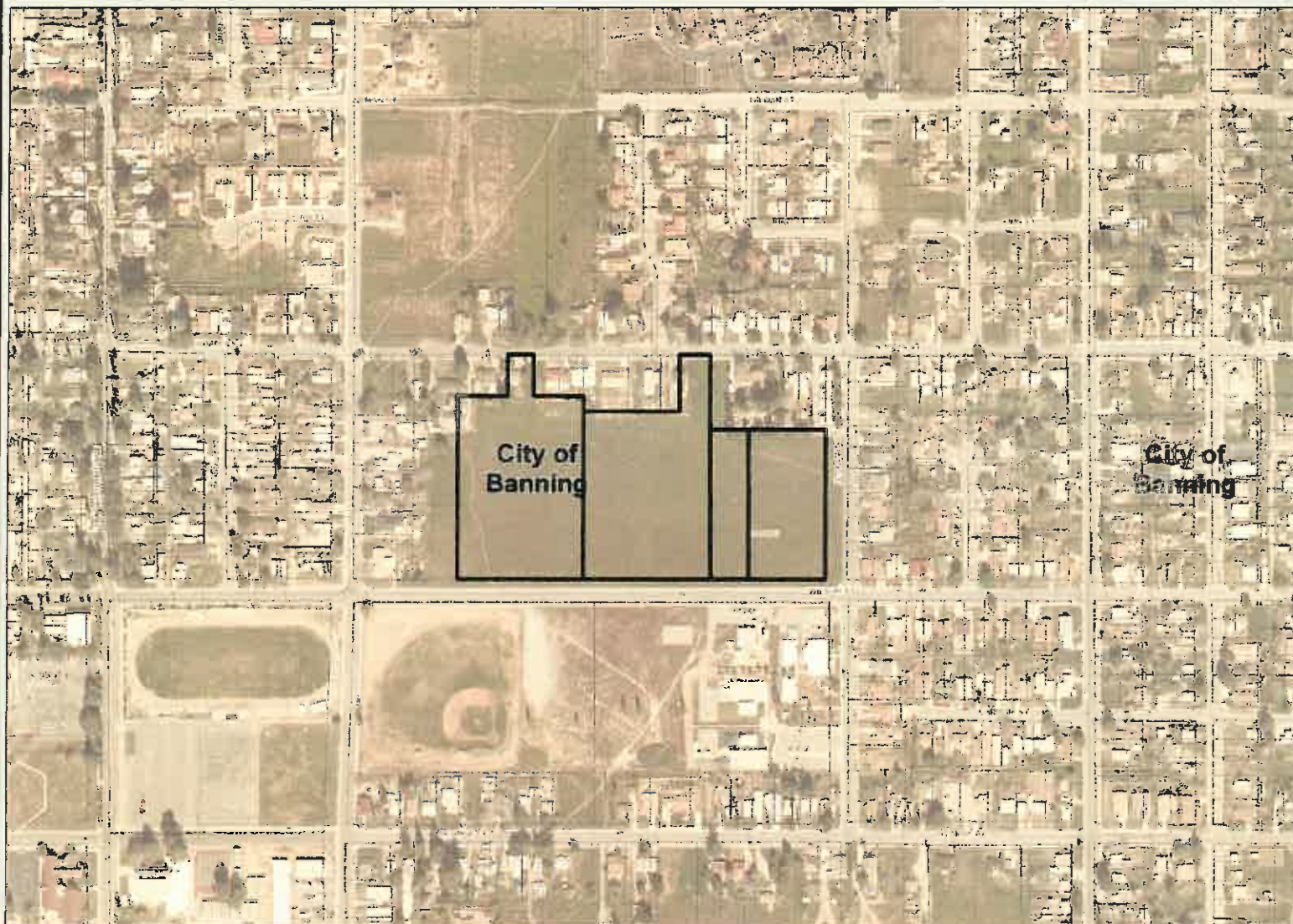
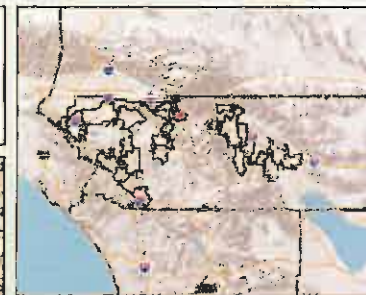
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/13/2015 10:41:44 AM

© Riverside County TLMA GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 503 1,007 Feet



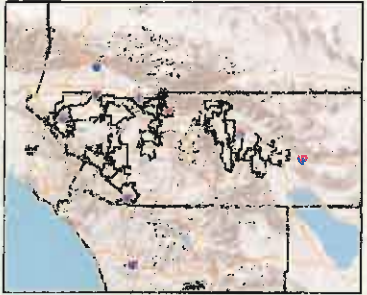
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 9/9/2016 2:33:41 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 252 503 Feet



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 9/9/2016 2:34:07 PM

© Riverside County RCIT GIS

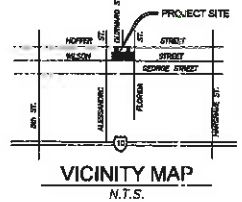
Notes

TENTATIVE TRACT MAP NO. 36710

BEING A SUBDIVISION OF A PORTION OF BLOCK 16 AND BLOCK 147, BANNING COLONY LANDS AS PER MAP RECORDED IN BOOK 3, PAGE 149 OF MAPS IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, AND IN BOOK 5 PAGE 186 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER, RECORDS OF RIVERSIDE COUNTY, CALIFORNIA.

UNITED ENGINEERING GROUP CA., INC

SEPTEMBER 2016



SITE PLANNING

GUIDELINES	COMMENTS
1. VARIOUS FOOTPRINTS	REPLIKER PARK VESTAS IS PROPOSING A SUBDIVISION CONSISTING OF SPANISH AND EARLY CALIFORNIA INSPIRED FLOOR PLANS WHERE PRIVATE COURTYARDS AND BALCONIES ARE EMPHASIZED.
2. VARIOUS SETBACKS	3 DIFFERENT FLOOR PLANS, WITH 3 DIFFERENT ELEVATIONS HAVE BEEN PROPOSED FOR THIS PROJECT, OFFERING VARIOUS FRONT AND SIDE YARD SETBACKS AND ELEVATIONS.
3. VARIOUS PLACEMENT OF HOUSES ON ADJACENT LOTS	REPLIKER PARK VESTAS CONSISTS OF 3 CUL-DE-SACS OFFERING A VARIETY OF LOT SIZES AND ORIENTATIONS THROUGHOUT THE PROJECT. THIS WILL ALLOW FOR WIDE VARIATION OF HOME PLACEMENTS ON EACH CUL-DE-SAC.
4. SLOWING VEHICULAR TRAFFIC	THE PROJECT CONTAINS PRIVATE STREETS AND THREE SEPARATE CUL-DE-SACS, THE LONGEST CUL-DE-SAC BEING 404 FEET, THIS COMBINED WITH THE SMALLER PRIVATE STREET SECTIONS WILL GREATLY REDUCE TRAFFIC SPEEDS WITHIN THIS PROJECT.
5. PREVENT STARK UNBROKEN WALLS	THE PROJECT WILL CONTAIN PERIMETER WALLS FOR PRIVACY AS WELL AS BLOCK WALLS BETWEEN HOMES TO MAINTAIN THE PRIVATE OPEN SPACE/COURTYARDS FOR EACH HOME. HOMES ADJACENT TO TRAIL AREAS WILL CONTAIN VIEW FENCES TO INCREASE VISIBILITY AND REDUCE CROWDING.
6. PLACEMENT AND MIX OF LOTS	THE SUBDIVISION CONTAINS 3 SEPARATE CUL-DE-SACS WHICH OFFER A WIDE MIX OF LOT SIZES AND ORIENTATIONS.
7. PRESERVATION OF WILDLIFE HABITAT AND NATURAL FEATURES	THE PROJECT IS IN AN OPEN AREA OF THE CITY ON A LOT THAT HAS BEEN GREATLY DISTURBED. BIOLOGICAL STUDIES OF THE AREA CONCLUDED NO HABITAT OR SENSITIVE FEATURES EXIST ON THE PROPERTY.
8. PRESERVATION OF VIEWS	THE NEIGHBORHOOD HAS GREAT VIEWS OF MT. SAN JACINTO, THE NATURAL CONTOURS OF THE PROJECT SLOPE FROM NORTH TO SOUTH WHICH WILL MAINTAIN VIEWSHEDS THROUGHOUT THE AREA.
9. TREATMENT OF DRAINAGE AND RUNOFF FROM RAIN	DRAINAGE FROM THIS SITE WILL BE TREATED IN A WATER QUALITY BASIN AT THE SOUTHEAST CORNER OF THE SITE. THE BASIN WILL ALSO BE USED AS A DOG PARK FOR AREA RESIDENCE AND WILL BE MAINTAINED BY THE DEVELOPER'S HOA.
10. WALL AND FENCE TREATMENTS	THE WALLS AND FENCES IN REPLIKER PARK VESTAS WILL BE BUILT IN CONFORMANCE WITH THE CONCEPTUAL LANDSCAPE PLANS FOR THE SITE. BLOCK WALLS WILL BE BUILT IN CONFORMANCE WITH PERIMETER AND VIEW FENCES WILL BE REQUIRED FOR LOTS ADJACENT TO THE TRAIL AREAS.

OPEN SPACE & AMENITIES

GUIDELINES	COMMENTS
1. PROVIDE AT LEAST THIRTY PERCENT OF THE TOTAL PROPERTY AS USABLE OPEN SPACE FOR PASSIVE AND ACTIVE RECREATIONAL USES.	REPLIKER PARK VESTAS PROVIDES A DOG PARK, PICNIC AREAS AND WALKING TRAIL THAT CONNECTS THROUGH THE MIDDLE OF THE PROJECT AND THROUGHOUT THE AREA ON EACH SIDE OF PRIVATE STREETS. IN TOTAL, THE PROJECT CONTAINS OVER 3 ACRES OF OPEN SPACE (20%).
2. USABLE OPEN SPACE SHALL NOT INCLUDE PORTIONS OF WAY, VEHICULAR PARKING AREAS ADJACENT TO OR BETWEEN STRUCTURES WHICH ARE LESS THAN FIFTEEN FEET APART, SETBACKS, PATIOS AND PRIVATE YARDS, OR SLOPE AREAS GREATER THAN EIGHT PERCENT.	LANDSCAPE AREAS AND OPEN SPACE WHICH ATTRIBUTE TO THE JOSS OPEN SPACE INCLUDE THE PICNIC AREAS, DOG PARK, WALKING TRAILS AND OTHER LANDSCAPE AREAS WHICH WILL BE MAINTAINED BY THE HOMEOWNERS ASSOCIATION. NO PRIVATE PATIO OR LOT AREA IS INCLUDED IN THIS CALCULATION.
3. USABLE OPEN SPACE THAT IS ENCLOSED BY AN UNDERLYING UTILITY EASSEMENT CAN BE INCLUDED IN THE OPEN SPACE REQUIREMENT, BUT SHALL NOT BE INCLUDED IN THE REQUIRED PARK LAND DEDICATION AS REQUIRED IN BANNING MUNICIPAL CODE CHAPTER 15.6B.	THE POWER LINE EASSEMENT TO SOUTHERN CALIFORNIA EDISON IS INCLUDED IN THE PROJECT OPEN SPACE CALCULATION. IT IS UNDERSTOOD THAT THIS AREA WILL NOT BE USED IN DETERMINING THE QUANTITY FEES.
4. PROVIDE RECREATIONAL AMENITIES WITH THE SITE SUCH AS: SWIMMING POOLS, CLUBHOUSE, TOT LOT W/ PLAY EQUIPMENT, COURT GAME FACILITIES FOR TENNIS, BASKETBALL, OR RACQUETBALL, IMPROVED SOFTBALL OR BASEBALL FIELDS, OR DAY CARE FACILITIES. THE TYPE OF AMENITIES SHALL BE APPROVED BY THE COMMUNITY DEVELOPMENT DEPARTMENT AND PROVIDED ACCORDING TO THE FOLLOWING SCHEDULE: 1-10 UNITS 1 AMENITY 11-50 UNITS 2 AMENITIES 51-100 UNITS 3 AMENITIES 101-200 UNITS 4 AMENITIES	THE LIMITED SIZE OF REPLIKER PARK VESTAS PRECLUDES A CLUBHOUSE OR SWIMMING POOL. THE PUB DOES PROVIDE A PICNIC AREA, BBQ GRILLS, TABLES, BENCHES, DOG PARK, AND TRAIL AREAS FOR ACTIVE RECREATION.

STREETS

GUIDELINES	COMMENTS
1. PRIVATE STREETS SHALL BE PERMITTED WHEN THERE IS A HOMEOWNERS ASSOCIATION ESTABLISHED TO MAINTAIN THEM. STREETS SHALL BE BUILT TO STANDARDS AND SPECIFICATIONS NO LESS THAN THOSE FOR PUBLIC WORKS CONSTRUCTION.	REPLIKER PARK VESTAS CONTAINS 3 CUL-DE-SACS AND IS PROPOSING PRIVATE STREETS. THESE STREETS WILL BE MAINTAINED BY A HOMEOWNERS ASSOCIATION. THE STREETS MEET PUBLIC WORKS STANDARDS AS WELL AS BANNING FIRE STANDARDS AND PARKING WILL BE LIMITED TO ONE SIDE OF THE STREET ONLY.

FIRE CODE

GUIDELINES	COMMENTS
1. ALL PARTS OF ANY STRUCTURES SHALL BE WITHIN 150 FEET OF PAVED ACCESS FOR SINGLE STORY RESIDENCES AND WITHIN 50 FEET OF PAVED ACCESS FOR MULTI-STORY RESIDENTIAL UNITS.	ALL STRUCTURES WITHIN THE REPLIKER PARK VESTAS DEVELOPMENT WILL HAVE DIRECT ACCESS TO PAVED STREETS.

LEGAL DESCRIPTION

PARCEL 7:
PER PRELIMINARY REPORT ORDER NO. 1083048 DATED JULY 25, 2011, AS PREPARED BY NORTH AMERICAN TITLE COMPANY, THE SUBJECT PROPERTY IS DESCRIBED AS FOLLOWS:
THE EASTERLY HALF OF BLOCK 15 OF BANNING COLONY LANDS, IN THE CITY OF BANNING, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 3, PAGE 149 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, CALIFORNIA, AND IN BOOK 5, PAGE 186 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF RIVERSIDE COUNTY, CALIFORNIA.

EXCEPT THE NORTHERLY 110.00 FEET OF THE WESTERLY 135.00 FEET AND THE NORTHERLY 110.00 FEET OF THE EASTERLY 135.00 FEET THEREON.
ALSO EXCEPT THE SOUTHERLY 30.00 FEET AS CONVEYED TO THE CITY OF BANNING, A MUNICIPAL CORPORATION.

PARCEL 2:
BLOCK 147 OF BANNING COLONY LANDS, IN THE CITY OF BANNING, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 3, PAGE 149 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, CALIFORNIA, AND IN BOOK 5, PAGE 186 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF RIVERSIDE COUNTY, CALIFORNIA.

EXCEPT THE SOUTHERLY 30.00 FEET AND THE EASTERLY 30.00 FEET AS CONVEYED TO THE CITY OF BANNING, A MUNICIPAL CORPORATION.
ALSO EXCEPT THE NORTHERLY 150.00 FEET OF THE WESTERLY 200.00 FEET AND THE NORTHERLY 200.00 FEET OF THE EAST HALF OF SAID BLOCK 147.

OWNER/APPLICANT:

1040 RESIDENTIAL 2010, LLP
4800 N. GARDEN CENTER DRIVE
SUITE 200
SCOTTSDALE, AZ 85258
PHONE: 480-609-1200
FAC: 480-609-1161

ENGINEER

UNITED ENGINEERING GROUP-CA, INC.
10822 TRUCEMARK Pkwy
SUITE 508
PACIFIC CALIFORNIA, CA 91730
PHONE: 909.465.9240
www.ueg.com

PROJECT DATA

EASTING ZONING	LDR
PROPOSED ZONING	R16
# OF LOTS	46
TOTAL SITE AREA (GROSS)	484,990 SF (10.87 AC)
TOTAL SITE AREA (NET)	442,253 SF (10.16 AC)
TOTAL OPEN SPACE AREA	142,850 SF (3.18 AC)
MINIMUM LOT SIZE	5,000 SF
HARROW LOT SIZE	12,817 SF
AVERAGE LOT SIZE	5,083 SF
PROJECT DENSITY (GROSS)	4.31 DU/AC
PROJECT DENSITY (NET)	4.33 DU/AC
OPEN SPACE PERCENTAGE	30.7%

LOT AREAS:

LOT #	SQ. FT.	ACRE	LOT #	SQ. FT.	ACRE
1	2,881	0.12	36	6,083	0.12
2	5,320	0.12	37	5,396	0.12
3	5,320	0.12	38	5,338	0.12
4	5,320	0.12	39	5,339	0.12
5	5,320	0.12	40	5,339	0.12
6	5,320	0.12	41	6,089	0.13
7	5,320	0.12	42	6,700	0.13
8	5,074	0.11	43	6,335	0.12
9	6,192	0.14	44	5,335	0.12
10	8,492	0.22	45	5,339	0.12
11	6,261	0.14	46	7,741	0.18
12	5,983	0.13	37	10,793	0.25
13	5,113	0.12	38	12,817	0.29
14	5,320	0.12	39	6,274	0.14
15	5,320	0.12	40	5,347	0.12
16	5,300	0.12	41	6,980	0.13
17	5,378	0.12	42	5,141	0.12
18	5,132	0.12	43	5,000	0.11
19	5,533	0.12	44	5,000	0.11
20	5,333	0.12	45	5,000	0.11
21	5,332	0.12	46	6,468	0.22
22	5,333	0.12			
23	5,332	0.12			
24	7,025	0.16			
25	6,822	0.15			
			TOTAL	279,157	6.41

LETTERED LOT AREAS:

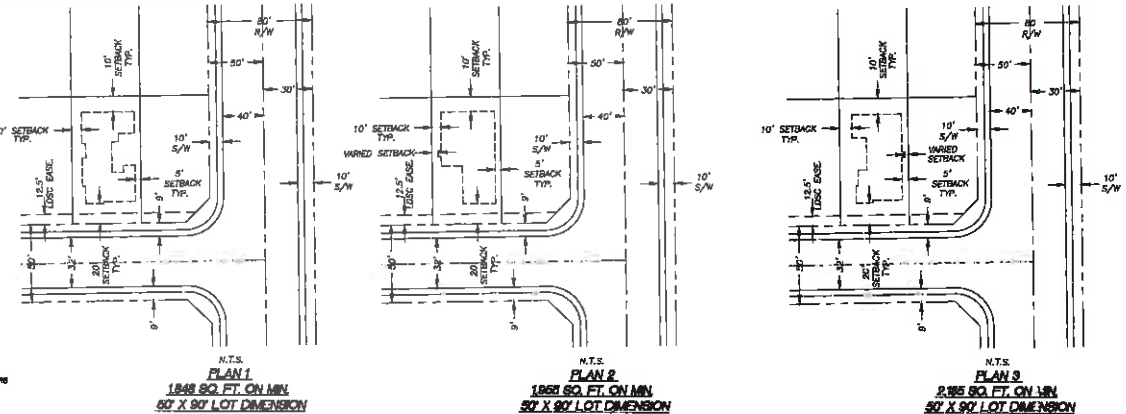
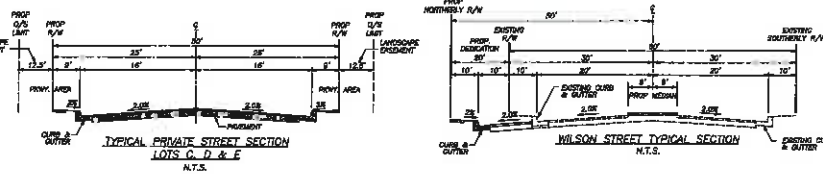
LOT #	SQ. FT.	ACRE
LOT A (BASIN)	33,057	0.76
LOT B (OPEN SPACE)	58,027	1.33
LOT C (PRIVATE STREET)	27,733	0.64
LOT D (PRIVATE STREET)	25,443	0.59
LOT E (PRIVATE STREET)	16,728	0.43

AREA TOTALS:

	SQ. FT.	ACRE
SPR LOTS (- PRNKY. AREA)	280,574	5.76
OPEN SPACES		
LOT A (BASIN)	33,057	0.76
LOT B	58,027	1.33
HARROW AREA	51,808	1.16
PRIVATE STREETS (AC PAVED)	45,856	1.12
WILSON STREET (DEDICATION)	22,787	0.52
TOTAL:	484,990	10.87

ZONING TABLE

ZONING DISTRICT	LDR/PUD
MAXIMUM BUILDING HEIGHT	TWO-STORY OR 35 FT
MINIMUM LOT AREA	5,000 SF
MINIMUM LOT WIDTH	50 FT
MINIMUM FRONT SETBACK	20 FT
MINIMUM SIDE SETBACK (FROM OTHER STRUCTURE)	15 FT
MINIMUM STREET SIDE SETBACK	15 FT
MINIMUM REAR SETBACK	15 FT
MINIMUM FENCE/WALL HEIGHT	6 FT
MINIMUM BUILDING COVERAGE	40%



SUBMITTALS	NO.	REVISIONS DESCRIPTION	DATE	BENCHMARK
	1	ADDRESS CITY COMMENTS	7/20/16	NO. 17 (7) REEST 024-2347.4 (0400 80) ADDRESS DISK IN TOP OF CURB, IN LIVINGSTON ST., 25.5 FT E/O CENTER OF SAN GORDON AVE. 88.5 FT W/O CENTER OF ALLEN ST. N/O CENTER OF LIVINGSTON ST. IN THE CITY OF BANNING.
DESIGNED BY: DCP				BASIS OF BEARING: THE BEARING IMPROVEMENT SHOWN AS THE CENTERLINE OF WILSON STREET ON E.S. 133716 WAS USED AS THE BASIS OF BEARING FOR THIS MAP.
DRAWN BY: RMG				
CHECKED BY:				

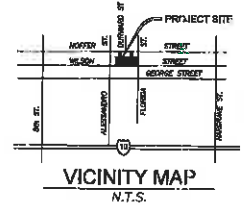
PREPARED UNDER THE DIRECT SUPERVISION OF:

 DEAN C. PHILLIPS DATE: SEPTEMBER 30, 2017

10822 TruceMark Pkwy
 Suite 508
 Pacific Califormnia,
 California 91734
 Phone 909.465.9240
 www.ueg.com

CITY OF BANNING
 TENTATIVE TRACT MAP NO. 36710
 DATE: SEPTEMBER 2, 2016
 SCALE: 1" = 50'
 SHEET 1 OF 2
 PROJECT NUMBER:
 C.A.-16809

IN THE CITY OF BANNING, STATE OF CALIFORNIA
TENTATIVE TRACT MAP NO. 36710
 BEING A SUBDIVISION OF A PORTION OF BLOCK 16 AND BLOCK 147, BANNING COLONY LANDS AS PER MAP RECORDED IN BOOK 3, PAGE 149 OF MAPS IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, AND IN BOOK 5 PAGE 186 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER, RECORDS OF RIVERSIDE COUNTY, CALIFORNIA.
 UNITED ENGINEERING GROUP CA, INC SEPTEMBER 2016



EASEMENT NOTES

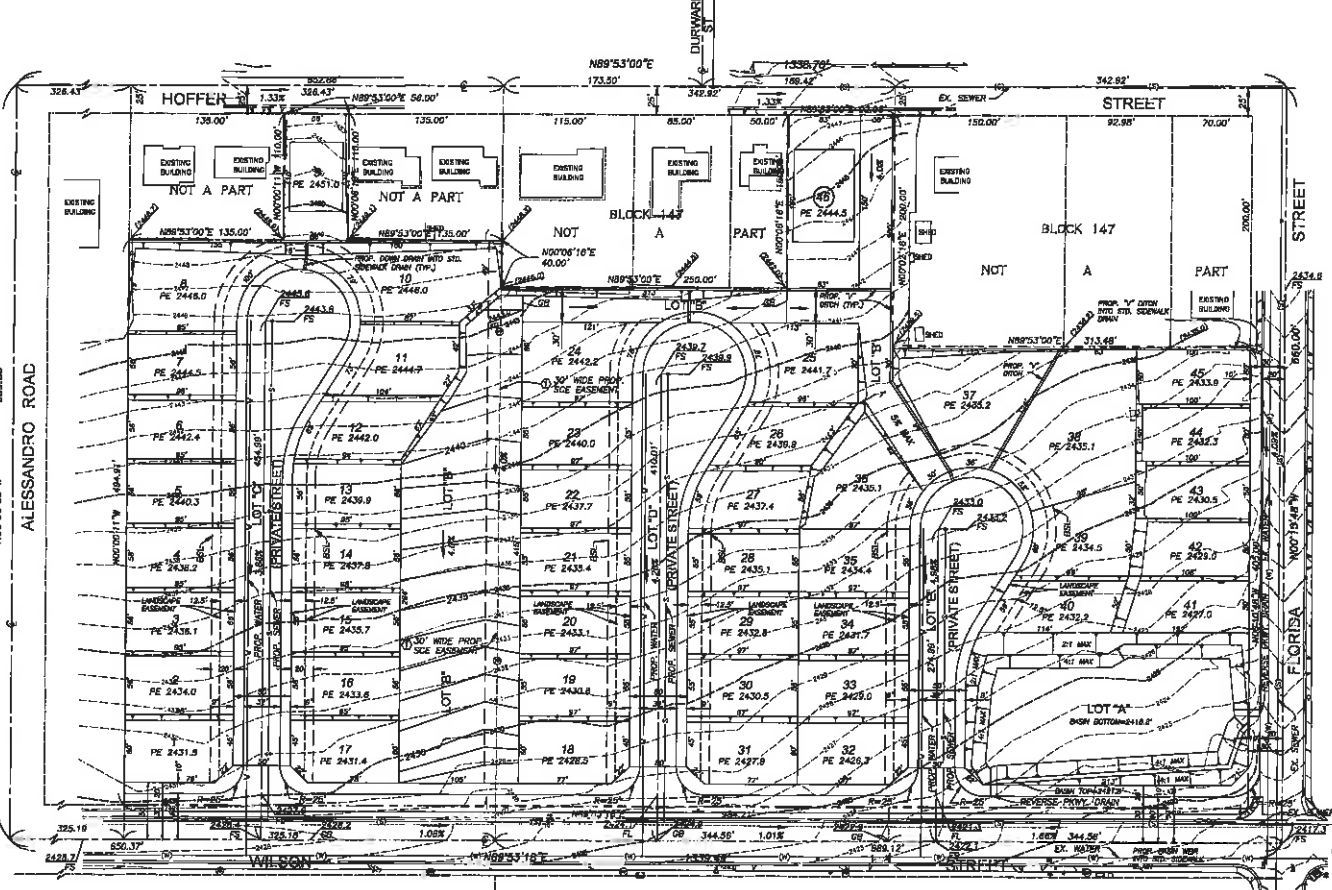
① AN EASEMENT FOR UTILITIES AND INCIDENTAL PURPOSES, AS GRANTED TO THE SOUTHERN CALIFORNIA POWER COMPANY BY DOCUMENTS RECORDED MARCH 18, 1914 IN BOOK 391, PAGE 137, AND IN BOOK 398, PAGE 274, BOTH OF RECORDS (PLATTED HERETO).

GENERAL NOTES:

- ASSESSOR'S PARCEL NUMBERS: 534-183-014, 534-200-004, 534-200-006 & 534-200-047
- THE PROPERTY SHOWN HEREIN CONTAINS THE ENTIRE CONTIGUOUS OWNERSHIP.
- TENTATIVE MAP AREA IS LOCATED IN THUMAS PLOTS MAP'S 2010 EDITION ON PAGE 722-GRID B2 & C2
- GENERAL PLAN & ZONING: EXISTING ZONING: LDR, PROPOSED ZONING: LDR-PUD
- EXISTING OR PROPOSED SURROUNDING ZONING: NORTH - LDR, SOUTH - PUBLIC FACILITIES, EAST - LDR, WEST - LDR
- EXISTING LAND USES: NORTH: SINGLE FAMILY RESIDENCE, SOUTH: VACANT & SCHOOL, EAST: SINGLE FAMILY RESIDENCE, WEST: SINGLE FAMILY RESIDENCE
- TOTAL GROSS AREA = 10.67 AC, GROSS DENSITY = 4.31 DU/AC, TOTAL NET AREA = 10.15 AC, NET DENSITY = 4.33 DU/AC (LESS ADDITIONAL WALSON ST. WIDENING)
- LOTS C, D & E ARE PRIVATE STREETS, WILSON STREET AND FLORIDA STREET ARE PUBLIC STREETS
- THE TOPOGRAPHY ON THIS MAP HAS A CONTOUR INTERVAL OF 1 FOOT AND WAS PROVIDED BY GUYETT GEOMATICS, INC.
- THE SITE IS IN FEMA FLOOD ZONE X.
- THERE ARE NO EXISTING STRUCTURES WITHIN THE PROPOSED IMPROVEMENTS.
- THE AREA IS NOT WITHIN A FAULT ZONE.

EARTHWORK QUANTITIES:

QUANTITIES ARE FOR TENTATIVE MAP PURPOSES ONLY AND ARE BASED ON THE PROPOSED GRADES SHOWN HEREON, A PROPOSED UNDERCUT SECTION FOR STREET IMPROVEMENTS AND THE GEOTECHNICAL ENGINEERING STUDY PREPARED BY EYEN, DATED APRIL 4, 2014. CONTRACTOR IS RESPONSIBLE FOR PREPARING HIS OWN QUANTITY TAKE-OFF AND BID RELATED TO GRADING QUANTITIES.
 CUT: APPROX. 20,588 CUBIC YARDS
 FILL: APPROX. 7,279 CUBIC YARDS
 IMPORT: 0 CUBIC YARDS
 EXPORT: 12,008 CUBIC YARDS
 THESE QUANTITIES ARE SUBJECT TO CHANGE.



UTILITY PURVEYORS:

- WATER:** CITY OF BANNING, 88 E. RAMSEY ST., BANNING, CA 92220, PHONE: 951.922.3105
SEWER: CITY OF BANNING, 88 E. RAMSEY ST., BANNING, CA 92220, PHONE: 951.922.3105
ELECTRIC: CITY OF BANNING, 88 E. RAMSEY ST., BANNING, CA 92220, PHONE: 951.922.3105
GAS: SOUTHERN CALIFORNIA GAS COMPANY, 80 E. RAMSEY ST., BANNING, CA 92220, PHONE: 800.427.2200
TELEPHONE: VERIZON, 3301 W. FLORIDA AVE., HEALD, CA 92549, PHONE: 951.925.2188
CABLE: TIME WARNER CABLE, 3002 S. HIGHLAND SPRINGS AVE. #108, BANNING, CA 92220, PHONE: 800.427.2200
SCHOOL DISTRICT: BANNING UNIFIED SCHOOL DISTRICT, 181 WEST WILLIAMS ST., BANNING, CA 92220

LEGEND:

- B/L BUILDING SETBACK LINE
- - - - F/L FLOW LINE
- - - - P/S POWER SURFACE
- - - - P/O POINT OF INTERSECTION GRADE
- L/P LOW POINT
- H/P HIGH POINT
- S/B SWALE BANK
- - - - E/G EXISTING ELEVATION
- G/L LOT NUMBER
- P/E PROPOSED ELEVATION
- S/L SLOPE UNLESS OTHERWISE NOTED
- - - - T/B TRACT BOUNDARY
- - - - E/W EXISTING WATER
- - - - P/S PROP. SEWER
- - - - P/W PROP. WATER
- P/A PROPOSED ADJUSTMENT
- E/W EXISTING WATER
- L/P LIGHT POLE
- M/B MILE BOX
- P/W POWER POLE
- D/W DRAIN
- S/W SANITARISE POINT
- T/W TREE
- M/W METEOR

NO.	REVISIONS	DATE
1	ADDRESS CITY COMMENTS	9/20/16

BENCHMARK:
 No. 4 71 REPER. 014-3347.4 (NAD 83) BRASS DISK IN TOP OF CURB IN LIVINGSTON ST. BESS PT. @ 0 CENTER OF SAN ROMANO AVE. 64.5 FT W/O CENTER OF ALLEY, 20.5 FT/O CENTER OF LIVINGSTON ST., IN THE CITY OF BANNING.
BASIS OF BEARING:
 THE BANNING MERIDIAN IS SHOWN AS THE COMPASS LINE OF WILSON STREET ON P.S. 133/18 WAS USED AS THE BASIS OF BEARING FOR THIS MAP.

PREPARED UNDER THE DIRECT SUPERVISION OF:
 DEAN C. PHILLIPS
 LICENSED LAND SURVEYOR
 No. 6974
 Exp. 9/30/17
 DATE: 9-06-2016
 P.L.S. NO. 6974
 EXPIRES: SEPTEMBER 30, 2017



10802 Tradewind Pkwy
 Suite 500
 Riverside, CA 92504
 California 91704
 Phone: 908.496.8240
 www.ueg.com

CITY OF BANNING
TENTATIVE TRACT MAP NO. 36710

DATE: SEPTEMBER 2, 2016
 SCALE: 1" = 50'
 SHEET 2 OF 2
 PROJECT NUMBER:
 CA-16809



LANDSCAPE MASTER PLAN



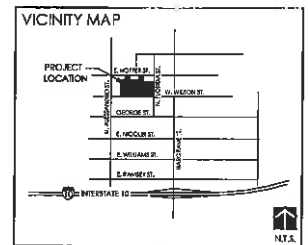
PASEO ENTRY

PLANT PALETTE

PLANT	COMMON NAME	HEIGHT	SPREAD
	MAGNOLIA G. CAMILLE SCHIMPER	SOUTHERN MAGNOLIA	AVERAGE 40' O.C. 24' BOK
	KOBORDIA BIPINNATA	CHINESE FLAME TREE	AVERAGE 40' O.C. 24' BOK
	LEMON LIME TREE	LEMON LIME TREE	ONE PER LOT 24' BOK
	LEMON LIME TREE	LEMON LIME TREE	ONE PER LOT 24' BOK
	LEMON LIME TREE	LEMON LIME TREE	ONE PER LOT 24' BOK

- NOTE:
- PERMANENT IRRIGATION SYSTEM SHALL BE INSTALLED ON ALL LANDSCAPED AREAS REQUIRING IRRIGATION. LOW WATER USE SYSTEMS SHALL BE INSTALLED WHERE APPROPRIATE. IRRIGATION SYSTEM SHALL BE DESIGNED TO PREVENT OVERWATERING OF PLANTS AND TO PREVENT WASTEFUL WATER USE.
 - ALL TREES SHALL BE MINIMUM CALIBER, TAPER, WEAR AND TEAR, AND BARK THICKNESS REQUIREMENTS SHALL BE MET.
 - SUCH TREES AS A 1" DBH OR GREATER IN VERTICAL HEIGHT WITH BODIES GREATER THAN OR EQUAL TO 31" TO BE LANDSCAPED AT A MINIMUM WITH AN APPROPRIATE GROUND COVER ONE (1) GALOPHLYDORUS SPECIES PER 100 SQUARE FEET OF SCAPE AREA, AND ONE (1) GALOPHLYDORUS SPECIES PER 100 SQUARE FEET OF SCAPE AREA. SUCH TREES IN DOUBLE ROWS SHALL BE PLANTED IN VERTICAL HEIGHT WITH BODIES GREATER THAN 31" SHALL ALSO BE PROVIDED WITH THE SAME GROUND COVER PER 100 SQUARE FEET OF SCAPE AREA IN ADDITION TO THE ABOVE REQUIREMENTS.
 - ALL LANDSCAPE AREAS SHALL RECEIVE 2" DEPTH SHREDDED BARK MULCH.
 - ALL UTILITIES SHALL BE SCREENED WITH APPROPRIATE PLANT MATERIAL.
 - ALL PUBLIC AND LANDSCAPED PARKING AND ON-SITE LIGHTING SHALL BE SCREENED BY THE PROPOSED PLANTING.
 - ALL BROWNTOP AREAS TO HAVE ACCEPTABLE SPACING INSTALLED TO ENHANCE PLANT MATERIAL SURVIVAL. A COMBINATION OF TREES, SHRUBS, AND GROUND COVER SHALL BE ACCOMPANIED BY LANDSCAPING PLANS. PLANTING SPECIFICATIONS SHALL BE AS FOLLOWS:
 TREES: SPACING 15' SQUARE FEET ACCEPTABLE FOR 20 PERCENT
 SHRUBS: 5' SQUARE FEET
 GRASS: 10' SQUARE FEET
 MULCH: 2" DEPTH
 ACCEPTABLE FOR 100 PERCENT COVERAGE IN ONE YEAR.
 - DRIP SYSTEMS SHOULD BE USED IN ALL AREAS EXCEPT IRRIGATION AND SHALL OPERATE ON A SCHEDULED BASIS.
 - NATIVE AND DROUGHT-TOLERANT PLANTS WILL BE INCORPORATED WHEREVER POSSIBLE.
 - THE APPLICANT REQUESTS THAT ALL PLANTINGS WILL NOT INTERFERE WITH UTILITIES, ADJACENT SITES, DRIVING SIGHTLINES AND TRAFFIC SIGHTLINES.
 - TREES SHALL BE PLANTED A MINIMUM OF 5' FROM EDGE OF DRIVEWAY, 1' FROM CURB, GAS MAINS OR OTHER UTILITIES AND A MINIMUM OF 10' FROM UTILITY POLES AND FIRE HYDRANTS.

NOTE:
SEE SHEET L-2 FOR WALL AND FENCE LAYOUT



LANDSCAPE MASTER PLAN

WILSON AND FLORIDA
 CITY OF BANNING
 RMV RESIDENTIAL 2010, LLP
 8800 N. GAINWAY CENTER DR., STE 255, SCOTTSDALE, AZ 85258
 PHONE (480) 609-1200 FAX (480) 609-1191

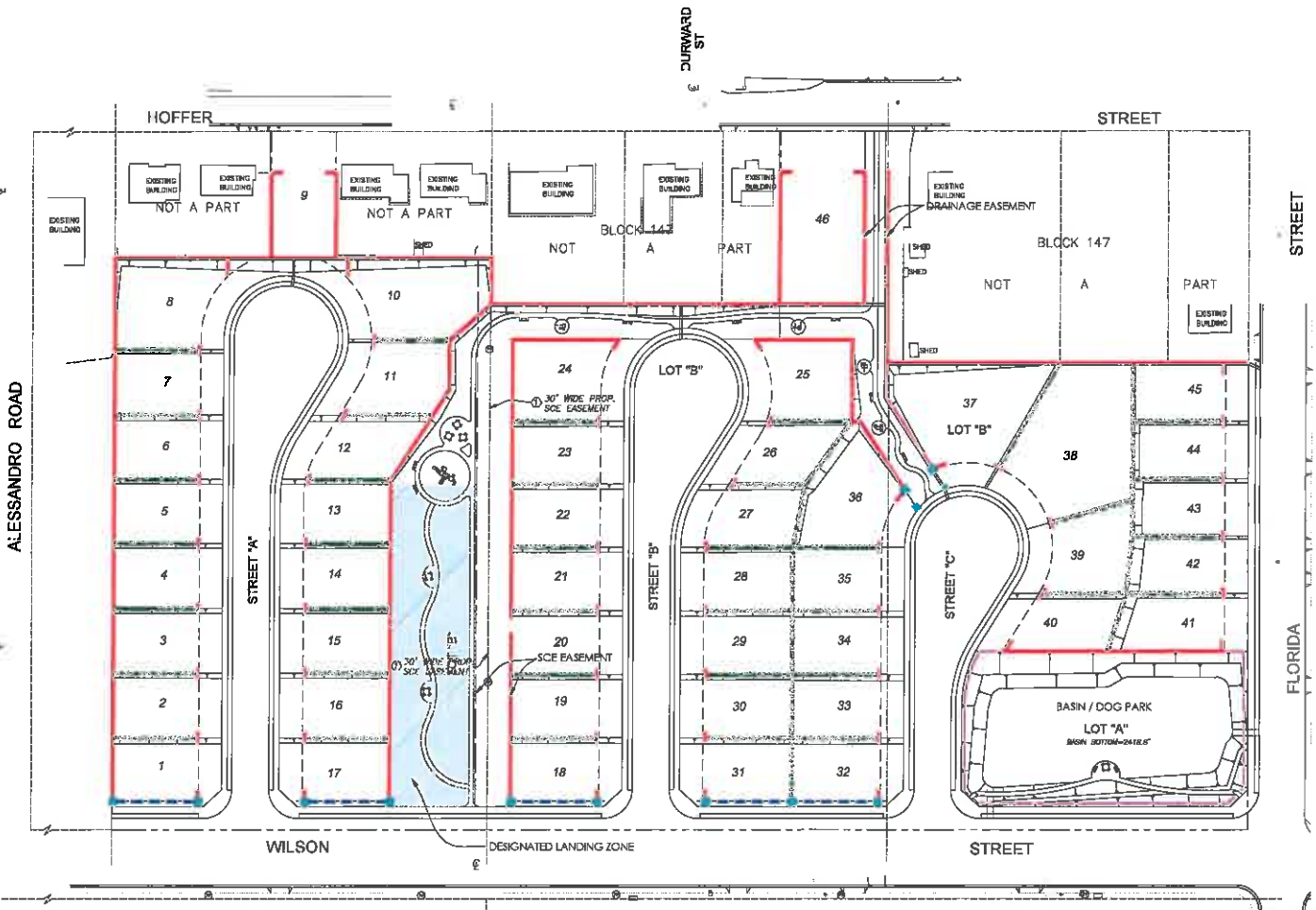


date: 03.14
 drawn: ESP
 checked: DEN
 sheet

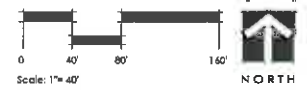
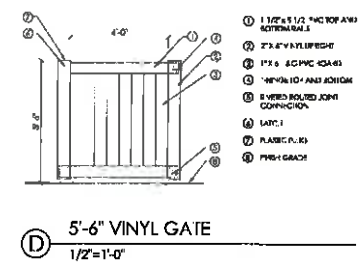
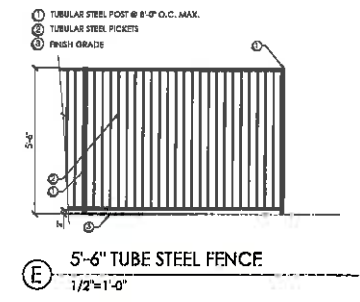
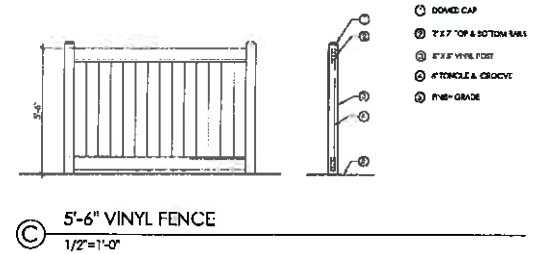
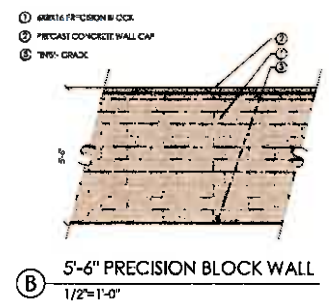
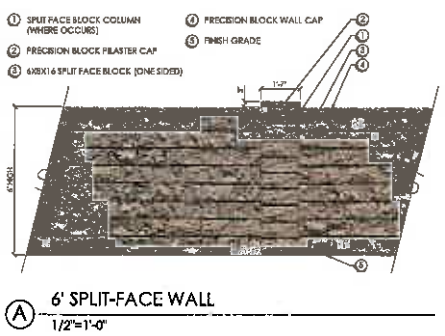


Date: 09.16
Drawn: SP
Checked: DJN
Scale:

SYMBOL	DESCRIPTION	DETAIL
	6" PERIMETER WALL - SPLIT FACE BLOCK - TAN	A
	5'-6" HIGH BLOCK WALL - PRECISION BLOCK - TAN	B
	5'-6" HIGH INTERIOR VINYL FENCE - WHITE	C
	5'-6" HIGH TUBE STEEL FENCE - BLACK	D
	MASONRY PLASTER - SPLIT FACE BLOCK - TAN	E
	SMALL MASONRY PLASTER (4" HIGH) - SPLIT FACE BLOCK - TAN (AT PARADE ENTRY - STREET 'C')	
	VINYL 24-RAIL FENCE - WHITE (AT PARADE ENTRY - STREET 'C')	



WALL AND FENCE PLAN





Replier Park Vistas

Additional Justification

3.3.1 Infill

This memo is being provided to the County of Riverside Airport Land Use Commission as supplemental justification to the originally submitted Developer's Statement for Replier Park Vistas. After additional review of ALUC's Countywide Policies, the applicant feels the project is also eligible for a Special Conditions consideration under Section 3.3.1 for infill projects. Per this section:

3.3.1a) A parcel can be considered for *infill* development if it meets *all* of the following criteria plus the applicable provisions of either Sub-policy (b) or (c) below:

- 1) The parcel size is no larger than 20.0 acres

Justification: The subject site is 10.67 acres gross.

- 2) At least 65% of the site's perimeter is bounded (disregarding roads) by existing uses similar to, or more intensive than, those proposed.

Justification: The subject property is surrounded by existing single family development to the north, east and west. These existing developments are all similar in size (5,000-7,000sf lots) to those being proposed on TTM 36710. To the south of there is an existing Child Care consortium building and an empty lot next to a baseball field. In total, the project perimeter measures 3,476.96' of which 1,997.84 (57%) is adjacent to existing single family homes and another 659.12' (19%) is adjacent to existing commercial development, totaling 76%. See Exhibit A attached

- 3) The proposed project would not extend the perimeter of the area defined by the surrounding, already developed, incompatible uses.

Justification: The subject project is on the boundary between Zone D and Zone E of the Banning Municipal Airport, with the existing development north of the property being in Zone E. Residential development in Zone E does not carry the requirement for 5du/ac therefore those developments are in conformance with the Banning Municipal Airports Land Use Plan.

- 4) Further increases in the residential density, nonresidential usage intensity, and/or other incompatible design or usage characteristics are prohibited.

Justification: The proposed density of project is 4.53 du/ac which is compatible if not less intense than the existing surrounding development in the area.



- 5) The area to be developed cannot previously have been set aside as open land in accordance with policies contained in this *Plan* unless replacement open land is provided within the same compatibility zone.

Justification: The subject property is zoned for single family residential development by the City of Banning and is privately owned. All the existing surrounding development took place prior to the adoption of the ALUC Land Use Compatibility Plan, therefore making it highly unlikely to be a part of any open space dedications.

- b) For residential development, the average development density of the site shall not exceed the lesser of:

- 1) The average density represented by all existing lot that lie fully or partially within a distance of 300 feet from the boundary of the parcel to be divided; or
- 2) Double the density permitted in accordance with the criteria for that location as indicated in the Compatibility Criteria matrix, Table 2A.

Justification: The proposed project has a density of 4.53 du/ac net. The average density of the surrounding lots within 300' is 4.81 du/ac net. (This density calculation excludes the large parcel in Zone E to the north and the commercial zoned properties to the south.) See "Exhibit B" attached

Table 2A of the Compatibility Criteria Matrix, indicates a target density of 5 du/ac gross. The proposed density is well under 10 du/ac.

300 Ft Buffer
4.81 Du/Ac

bing

Wilson & Florida

Banning, California



Exhibit D



UNITED ENGINEERING GROUP

Repplier Park Vistas
(Tract 36710)

Developer's Statement

County of Riverside
Airport Land Use Commission

September 2016

Prepared for



RMG Residential 2010, LLLP
8800 N. Gainey Center Drive
Suite 255
Scottsdale, AZ 85258

Replier Park Vistas

Submitted to

**County of Riverside
Airport Land Use Commission**

September 2016

PROPERTY OWNER

RMG Residential 2010, LLLP
8800 N. Gainey Center Dr.
Suite 255
Scottsdale, AZ 85258
(480) 609-1200
Contact: Randall Andrus

AGENT/ENGINEER

UNITED ENGINEERING GROUP
10602 Trademark Pkwy
Suite 509
Rancho Cucamonga, CA 91730
(909) 466-9240
Contact: Beau Cooper

UEG Project No. 16809

TABLE OF CONTENTS

1.0	PURPOSE OF REQUEST	4
2.0	PROJECT SUMMARY	6
3.0	CLOSING	7

FIGURES

Figure 1 – Project Vicinity	4
Figure 2- Banning General Plan/Zoning	5
Figure 3- Airport Land Use Map.....	5
Figure 4- Surrounding Density	6

APPENDIX

- Appendix A - Surrounding Density Study
- Appendix B - Tentative Tract Map
- Appendix C - Landscape Plans
- Appendix D - FAA Determination of No Hazard to Air Navigation

1.0 PURPOSE OF REQUEST

Repplier Park Vistas (the *Project*) is a property containing 10.68 acres and is located on the north side of Wilson Street, west of Florida Street and south of Hoffer Street in the City of Banning. (See *Figure 1 – Project Vicinity*) The assessor’s parcel numbers for this project are: 534-183-014; 534-200-004, -008 & -047.

The property is zoned LDR in the City of Banning (See *Figure 2 – Banning General Plan/Zoning*), which allows for a density of 0-5du/ac and has minimum lot sizes of 7,000 square feet. The property is also located within Zone D of the Banning Municipal Airport’s Land Use Map. (See *Figure 3 – Airport Land Use*) Zone D requires proposed projects to contain a density of 5du/acre in infill areas and also requires open space areas within the project to be utilized as potential emergency landing areas.

In order to comply with both the City and Airport Land Use Commission’s (ALUC) requirements, the Repplier Park Vistas project is proposing a Planned Unit Development (PUD) and Tentative Tract Map (TTM) containing single family residential lots with a minimum lot size of 5,000 square feet. The proposed TTM contains 46 lots on 10.22 net acres, giving it a density of 4.50du/ac. The PUD contains approximately 3.27 acres (30.6%) of usable open space area, including a neighborhood park, walking trails, picnic area and dogpark.



Figure 1 – Project Vicinity

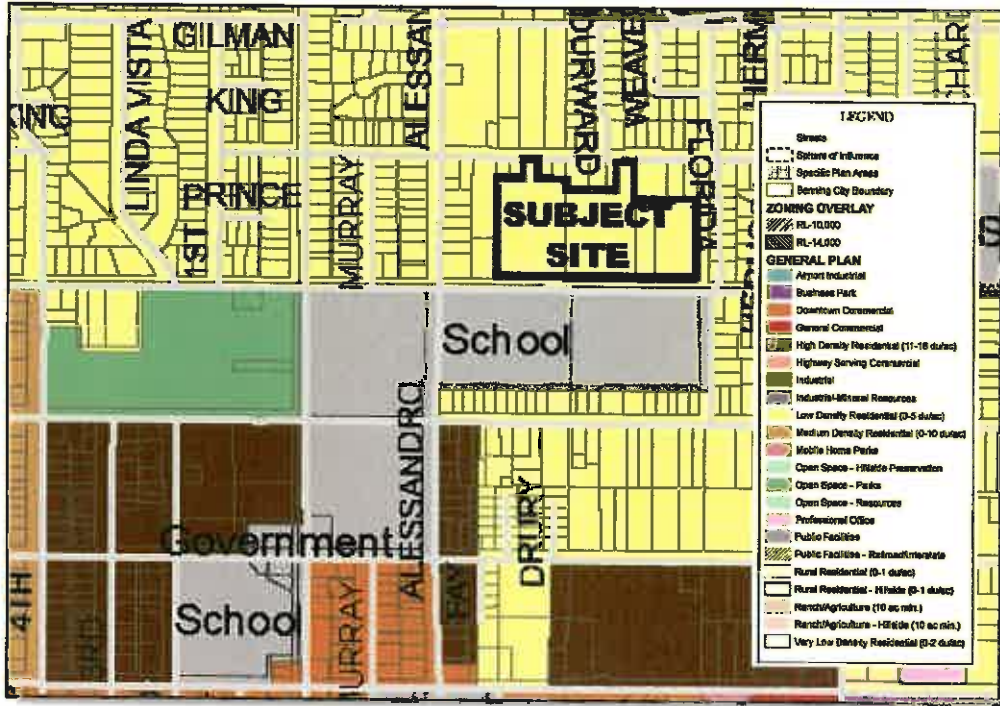


Figure 4- Banning General Plan/Zoning

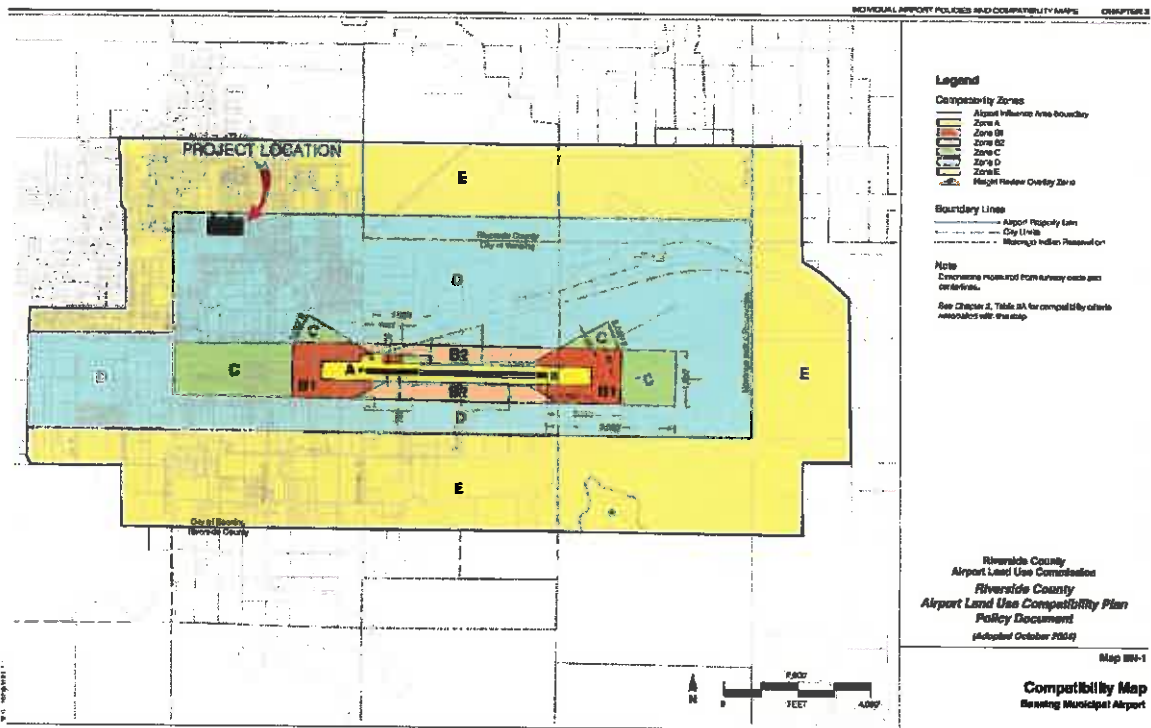


Figure 3 - Airport Land Use Map

2.0 PROJECT SUMMARY

The Replier Park Vistas project is seeking approval of a Tentative Tract Map and Planned Unit Development for 46 lots on 10.67 gross acres (4.31du/ac). The minimum lot size in this proposed project is 5,000 square feet, as allowed in Section 17.08.170 of Banning's Zoning Ordinance.

Also, part of Banning's PUD ordinance is the requirement for 30% of the project area to be open space & amenities. The Replier Park Vistas project has combine this requirement with ALUC's requirements for open space in developing the project's layout.

Although the project meets the open space requirements for ALUC, it cannot reach the density criteria of 5du/ac as set forth in the ALUC Compatibility Plan. Furthermore, it is important for the project to remain compatible to the surrounding development and character of the neighborhood. A vast majority of this neighborhood was built well before the adoption of Banning Airport's Compatibility Plan. In a study done by UEG of the surrounding area, none of the subdivisions meet the current 5du/ac requirement nor do they provide the open space required. (See **Figure 4 & Appendix A**)



Figure 4 – Surrounding Density

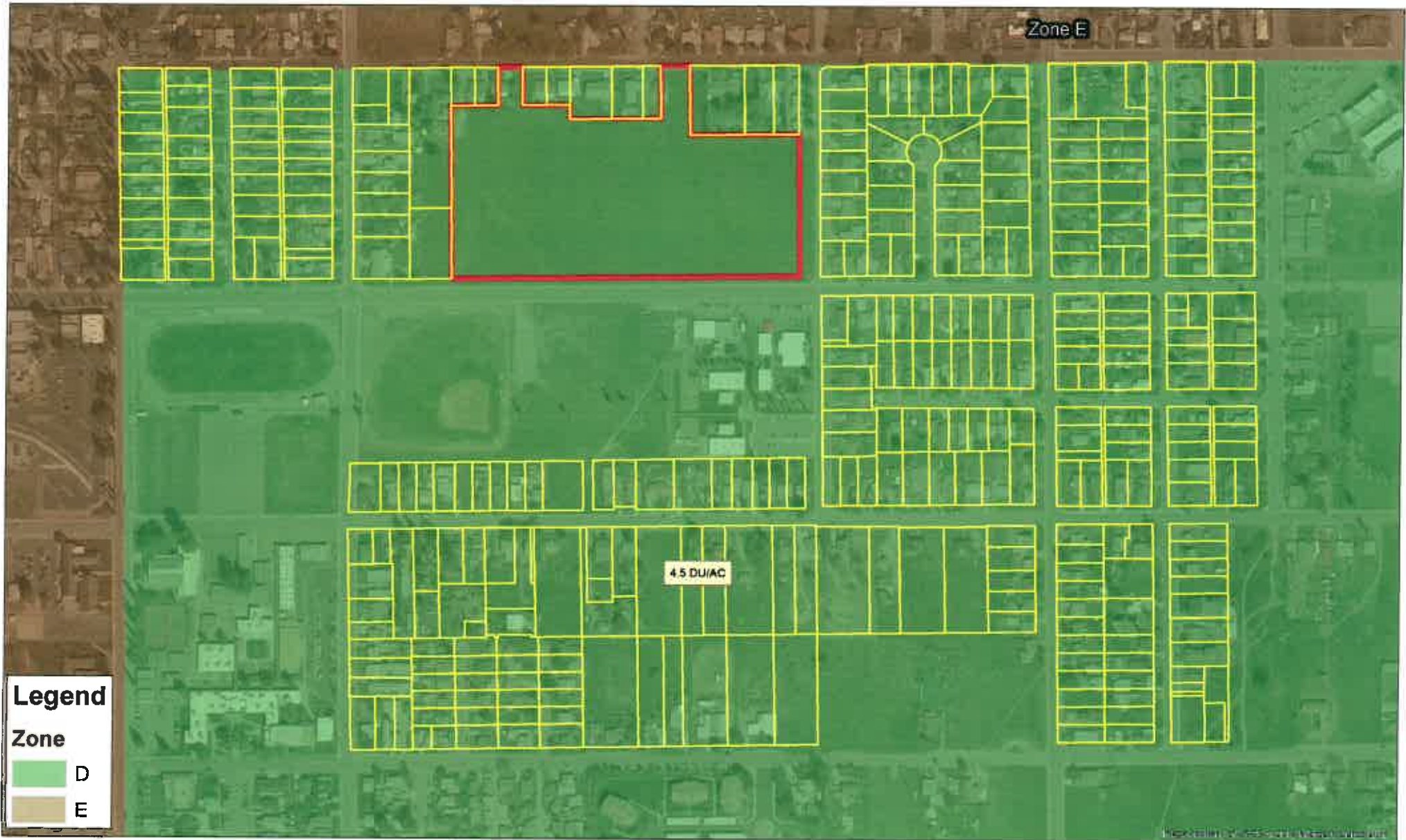
In order to reach the density goal of 5du/acre, the site would need to process a re-zone through the City of Banning and would need to propose a cluster development consisting of townhomes or duplexes. This type of use would be incompatible with the rest fo the surrounding neighborhood and likely not supported by the City of Banning.

Lastly, the project has made its application to the FAA and has received a "Determination of No Hazard to Air Navigation" from the FAA.

3.0 CLOSING

In an effort to remain compatible with the existing land uses surrounding this property, the Repplier Park Vistas project is seeking a determination of compatibility from the Riverside County Airport Land Use Commission, as allowed in Section 3.3.6 – Other Special Conditions of the Compatibility Plan, to allow for this 46 lot infill development, on 10.67 acres.

APPENDIX A



NOTICE OF PUBLIC HEARING

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Thursday from 8:00 a.m. to 5:00 p.m., except Thursday, November 24 (Thanksgiving Day), and by prescheduled appointment on Friday, December 2, from 8:30 a.m. to 5:00 p.m. Please send any written comments to this address.

ATTENTION: ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. The City of Banning may hold hearings on this project and should be contacted on non-ALUC issues.

PLACE OF HEARING: Eastern Municipal Water District
2270 Trumble Road, Board Chambers
Perris, California
[Hearing venue only – do not send written comments to this address]

DATE OF HEARING: December 8, 2016

TIME OF HEARING: 9:00 A.M.

CASE DESCRIPTION:

ZAP1020BA15 – RMG Residential 2010, LLLP (Representative: United Engineering Group) – City of Banning Planning Case No. 15-70004 (Tentative Tract Map No. 36710). The applicant is proposing to divide 10.67 acres (Assessor's Parcel Numbers 534-183-014, 534-200-004, 534-200-008, and 534-200-047) into 46 single-family residential lots, one drainage basin lot, and one open space lot. The project site is located on the northerly side of Wilson Street, westerly of Florida Street, southerly of Hoffer Street and easterly of Alessandro Road in the City of Banning (Airport Compatibility Zone D of the Banning Municipal Airport Influence Area).

FURTHER INFORMATION: Contact Paul Rull at (951) 955-6893. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Ms. Patty Nevins of the City of Banning Planning Department at (951) 922-3152.

APPLICATION FOR MAJOR LAND USE ACTION REVIEW
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

ALUC Identification No.

ZAPI020BARIS

PROJECT PROPONENT (TO BE COMPLETED BY APPLICANT)

Date of Application 9/24/15

Property Owner RMG Residential 2010, LLLP Phone Number (480) 609-1200

Mailing Address 8800 N. Gainey Center Drive, Suite 255
Scottsdale, AZ 85258

Agent (if any) United Engineering Group Phone Number (909) 466-9240 x203

Mailing Address 10602 Trademark Parkway, Suite 509
Rancho Cucamonga, CA 91730

PROJECT LOCATION (TO BE COMPLETED BY APPLICANT)

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address NWC of E Wilson Street and N Florida Street

Assessor's Parcel No. 534-183-014; 534-200-004, -008 & -047 Parcel Size 10.67 AC

Subdivision Name Replier Park Vista

Lot Number 46 SF Zoning Classification LDR

PROJECT DESCRIPTION (TO BE COMPLETED BY APPLICANT)

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe) Vacant land immediately surrounded by single family residential units

Proposed Land Use (describe) 46 single-family residential lots on 10.67 acres

For Residential Uses Number of Parcels or Units on Site (exclude secondary units) 46

For Other Land Uses Hours of Use _____

(See Appendix C) Number of People on Site Maximum Number _____
 Method of Calculation _____

Height Data

Height above Ground or Tallest Object (including antennas and trees) 35 ft.

Highest Elevation (above sea level) of Any Object or Terrain on Site 2486 ft.

Flight Hazards Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight? Yes No

If yes, describe _____

*Handing
D*

REFERRING AGENCY (APPLICANT OR JURISDICTION TO COMPLETE)	
Date Received	
Agency Name	City of Banning
Staff Contact	Brian Guillot
Phone Number	951-922-3152
Agency's Project No.	15-45000002 - Tract Map
Type of Project	<input type="checkbox"/> General Plan Amendment <input type="checkbox"/> Zoning Amendment or Variance <input checked="" type="checkbox"/> Subdivision Approval <input type="checkbox"/> Use Permit <input type="checkbox"/> Public Facility <input type="checkbox"/> Other

A. NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. SUBMISSION PACKAGE:

ALUC REVIEW

- 1. Completed Application Form
- 1. Project Site Plan – Folded (8-1/2 x 14 max.)
- 1. Elevations of Buildings - Folded
- 1 Each . 8 ½ x 11 reduced copy of the above
- 1. 8 ½ x 11 reduced copy showing project in relationship to airport.
- 1 Set . Floor plans for non-residential projects
- 4 Sets. . Gummed address labels of the Owner and representative (*See Proponent*).
- 1 Set. . Gummed address labels of all property owners within a 300' radius of the project site. If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10), with ALUC return address.
- 4 Sets. . Gummed address labels of the referring agency (City or County).
- 1. Check for Fee (See Item "C" below)

STAFF REVIEW (Consult with ALUC staff planner as to whether project qualifies)

- 1. Completed Application Form
- 1. Project Site Plans – Folded (8-1/2 x 14 max.)
- 1. Elevations of Buildings - Folded
- 1. 8 ½ x 11 Vicinity Map
- 1 Set . Gummed address labels of the Owner and representative (*See Proponent*).
- 1 Set . Gummed address labels of the referring agency.
- 1. Check for review–See Below

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.4

HEARING DATE: December 8, 2016

CASE NUMBER: ZAP1066BD16 – Fiesta Ford, Inc. (Representative: KSC, Inc.)

APPROVING JURISDICTION: City of Indio

JURISDICTION CASE NO: 16-10-403 (Design Review), 16-10-1018 (Conditional Use Permit)

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends that the Commission find the Design Review and Conditional Use Permit **CONDITIONALLY CONSISTENT**, subject to the conditions included herein, and such additional conditions as may be required by the Federal Aviation Administration Obstruction Evaluation Service.

PROJECT DESCRIPTION: The applicant is proposing to develop a Fiesta Ford Quick Lane and Collision Center auto repair facility consisting of a 10,314 square foot service repair building and 17,826 square foot collision building on 2.8 acres. The maximum building height is 32 feet.

PROJECT LOCATION: The site is located on the southeast corner of Avenue 40 and Adams Street in the City of Indio, approximately 2,360 feet northwesterly of runway 10-28 at Bermuda Dunes Airport.

LAND USE PLAN: 2004 Bermuda Dunes Airport Land Use Compatibility Plan

- a. Airport Influence Area: Bermuda Dunes Airport
- b. Land Use Policy: Compatibility Zone B1, C, D
- c. Noise Levels: 55 - 60 CNEL contour

BACKGROUND:

Non-Residential Average Intensity: Pursuant to the 2004 Bermuda Dunes Airport Land Use Compatibility Plan, the project site is primarily located within Compatibility Zones C and D. The site intersects a small portion of Compatibility Zone B1, which only contains street landscaping. Zone C

restricts average intensity to 75 people per acre and Zone D restricts average intensity to 100 people per acre. Approximately 1.3 acres of the site are located within Compatibility Zone C and 1.5 acres are located within Compatibility Zone D.

The “Building Code Method” for calculating intensity utilizes “minimum floor area per occupant” criteria from the Building Code as a factor in projecting intensity. Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, the following intensities were utilized for the project:

- office areas – 1 person/200 square feet;
- customer areas – 1 person/60 square feet;
- repair areas – 1 person/200 square feet; and
- storage areas – 1 person/300 square feet.

The overall project includes 17,925 square foot service repair area, 244 square foot office area, 1,510 square foot customer area, and 7831 square foot storage area, resulting in an overall occupancy of 142 people, and average intensity of 51 people per acre for the entire site. However, the project site is split between Compatibility Zones C and D, with 1.32 acres located within Zone C, and 1.53 acres located within Zone D. Approximately 9,804 square feet of building area (entirely within the Quick Lane auto repair building) are located within Compatibility Zone C, and 18,527 square feet of building area are located within Compatibility Zone D (primarily within the Collision repair body shop, but also including a 513 square foot portion of the waiting room inside the Quick Lane building).

A breakdown of use by occupancy located in Compatibility Zone C would result in 1 person for office area, 9 people for customer area, 10 people for storage area, and 29 people for repair area, resulting in an occupancy of 49 people, and average intensity of 37 people per acre, which is consistent with the Compatibility Zone C average intensity criterion of 75 people.

A breakdown of use by occupancy located in Compatibility Zone D would result in 1 person for office area, 17 people for customer area, 16 people for storage area, and 61 people for repair area, resulting in an occupancy of 95 people, and average intensity of 62 people per acre, which is consistent with the Compatibility Zone D average intensity criterion of 100 people.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per standard vehicle). Based on the number of standard parking spaces provided of 152 spaces, the total occupancy for both buildings would be 228 people, which results in an overall average intensity of 81 persons per acre. However, in this case, 74 spaces are behind a gate and are for employee parking and vehicles being repaired, not customers visiting the facility. This leaves 78 spaces for customers. Assuming 25 employees plus 1.5 persons per customer parking space, the total occupancy would be estimated at 142. The gated parking spaces located in Zone D are to be used as a temporary vehicle holding area where vehicles that are not being worked on are stored temporarily until such

time as they are brought into the shop for repairs.

Non-Residential Single-Acre Intensity: Pursuant to the 2004 Bermuda Dunes Airport Land Use Compatibility Plan, the project site is primarily located within Compatibility Zones C and D. The site intersects a small portion of Compatibility Zone B1, which only contains street landscaping. Zone C restricts single acre intensity to 150 people per acre, and Compatibility Zone D restricts single acre intensity to 300 people per acre.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre area would include the entire body shop building consisting of 144 square foot office area, 484 square foot customer area, 4,684 square foot storage area, and 12,125 square foot repair area. This would result in 86 people which is consistent with Compatibility Zone D single-acre criterion of 300.

Prohibited and Discouraged Uses: The applicant does not propose any uses specifically prohibited or discouraged in Compatibility Zones B1, C and D of the Bermuda Dunes Airport Influence Area.

Noise: The site is located within the 55-60 CNEL contour range from aircraft noise. As a primarily industrial use not sensitive to noise, the auto repair facility would not require special measures to mitigate aircraft-generated noise.

Part 77: The elevation of Runway 10-28 at its westerly terminus is approximately 73 feet above mean sea level (AMSL). At a distance of approximately 2,360 feet from the runway, FAA review would be required for any structures with top of roof exceeding 96.6 feet AMSL. The project's site elevation is 80 feet AMSL, and the height of the building is 32 feet, for a maximum top point elevation of 112 feet AMSL. Therefore, Federal Aviation Administration (FAA) obstruction evaluation review for height/elevation reasons is required. The applicant has submitted Form 7460-1, and FAAOES has assigned Aeronautical Study No. 2016-AWP-10807-OE to this project.

Open Area: The site is located within Airport Compatibility Zones B1, C and D of the Bermuda Dunes Airport Influence Area, which requires projects 10 acres or larger to designate 20% (in Zone C) and 10% (in Zone D) of project area as ALUC-qualifying open area that could potentially serve as emergency landing areas. Since the overall project size is less than 10 acres, the open area requirement is not applicable to this project.

CONDITIONS:

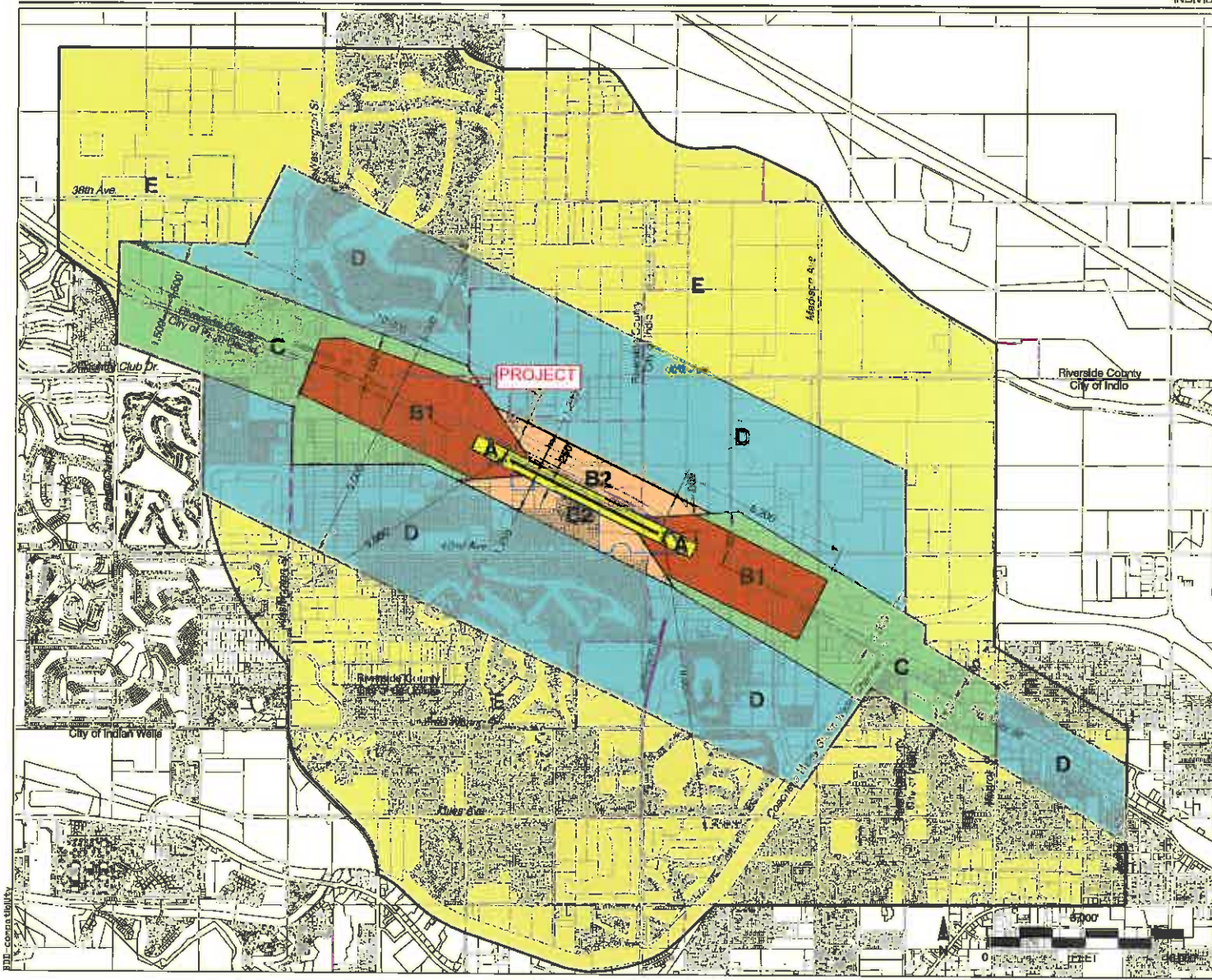
1. Any outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use or activity which would direct a steady light or flashing light of red, white,

green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.

- (b) Any use or activity which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use or activity which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, composting operations, production of cereal grains, sunflower, and row crops, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, day care centers, libraries, hospitals, nursing homes, and highly noise-sensitive outdoor nonresidential uses.
3. The attached notice shall be given to all prospective purchasers and/or tenants of the property, and shall be recorded as a deed notice.
4. Any new detention basin(s) on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Legend

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E

Boundary Lines

- Airport Property Line
- City Limits

Note

Southwestern edge of Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

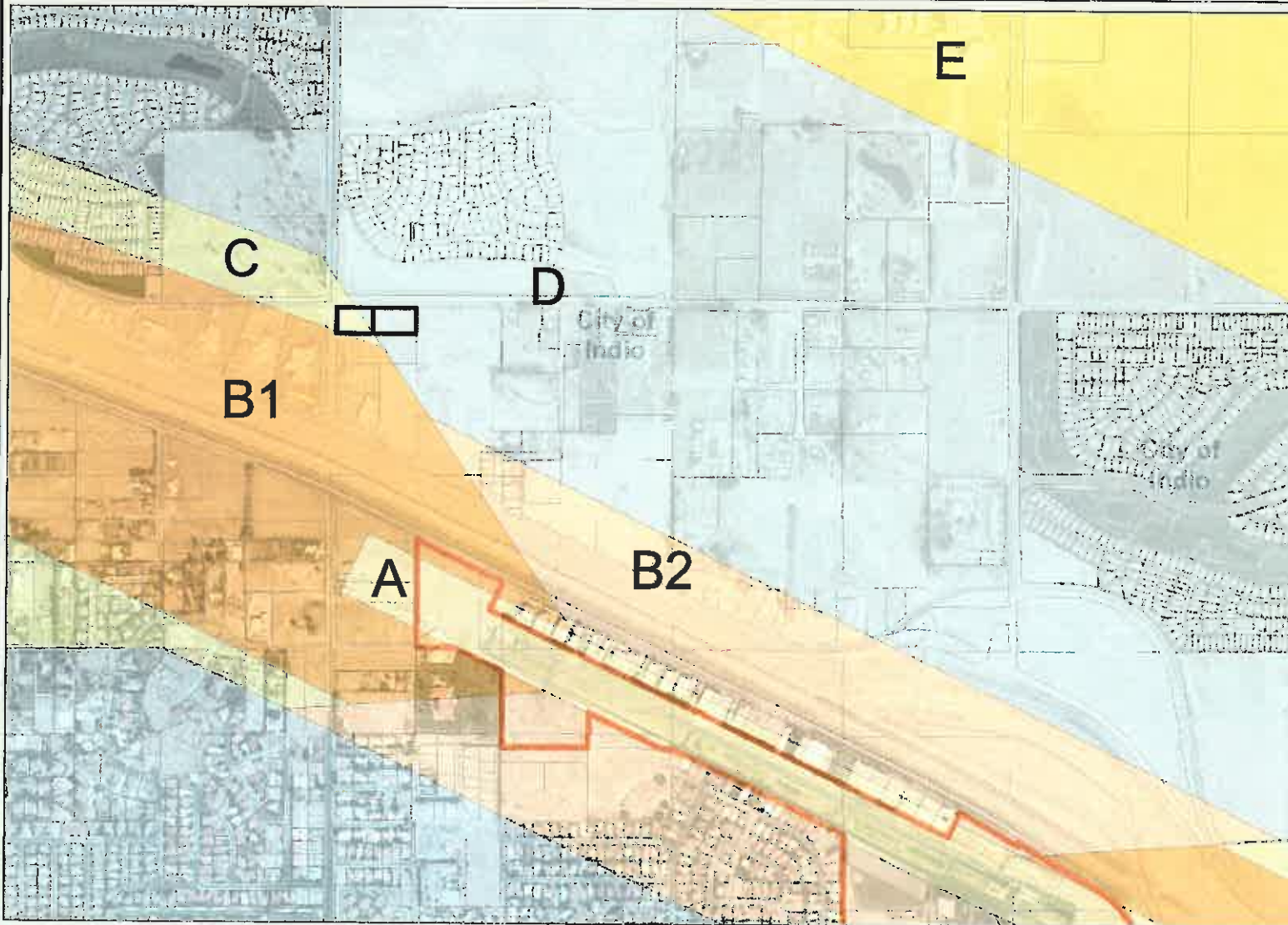
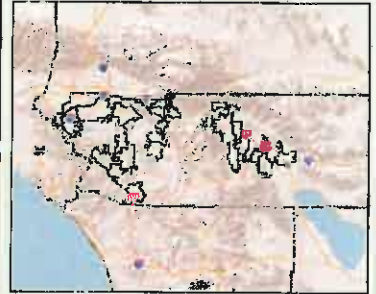
See Chapter 2, Table 2A for compatibility criteria associated with this map.

Riverside County
Airport Land Use Commission
Riverside County
Airport Land Use Compatibility Plan
Policy Document
 (Adopted December 2004)

Map BD-1

Compatibility Map
Bermuda Dunes Airport

My Map



Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



0 1,431 2,862 Feet



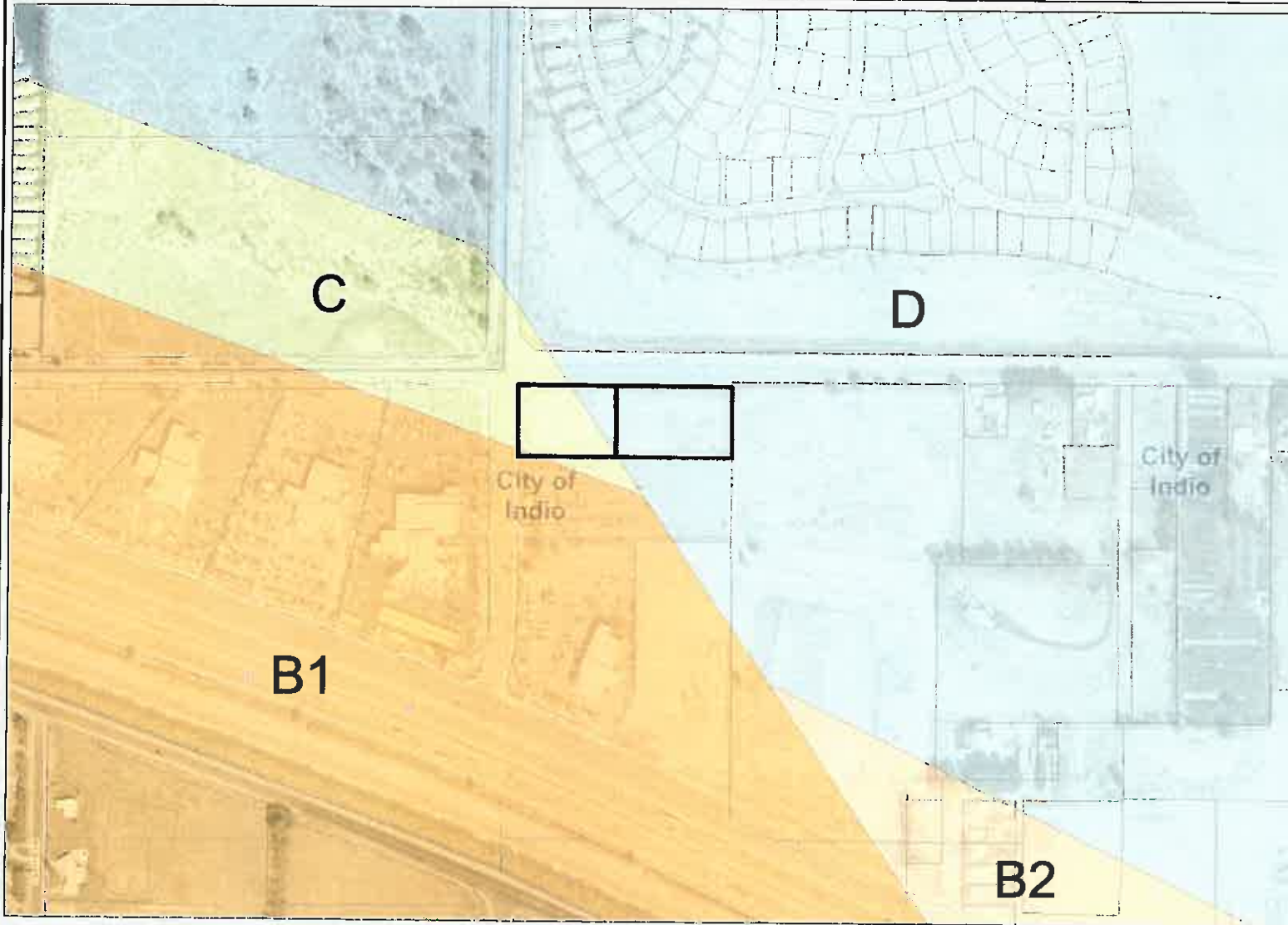
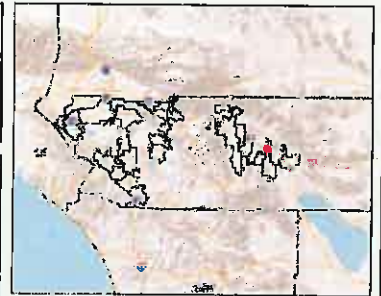
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/2/2016 12:02:11 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-FXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



0 527 1,055 Feet



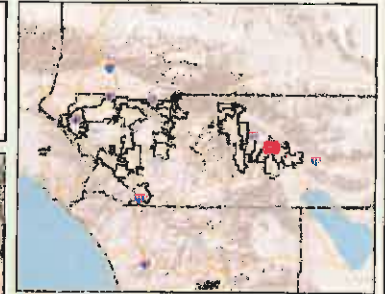
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/2/2016 11:59:28 AM

© Riverside County RCIT GIS

Notes

My Map



Legend

- City Boundaries
- Cities
- roads
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities
- hydrography
- lines
- waterbodies
- Lakes
- Rivers



0

2,862

5,724 Feet



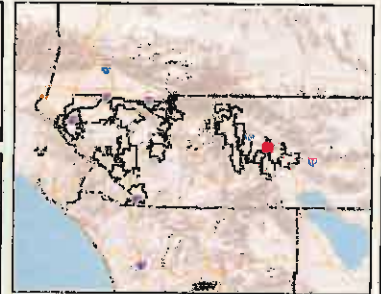
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/2/2016 12:04:05 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

0 1,431 2,862 Feet

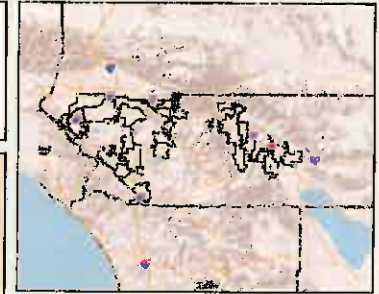


REPORT PRINTED ON... 11/2/2016 12:04:38 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 527 1,055 Feet



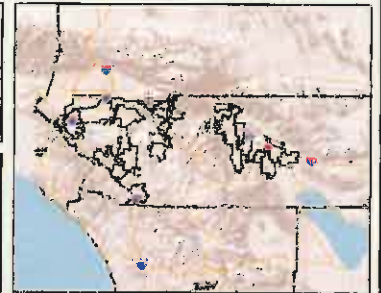
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/2/2016 12:00:17 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 358 716 Feet



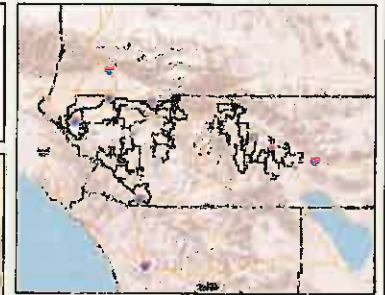
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/2/2016 12:05:50 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- City Boundaries
- Cities
- roadsanno
- highways
 - HWY
 - INTERCHANGE
 - INTERSTATE
 - OFFRAMP
 - ONRAMP
 - USHWY
- counties
- cities
- hydrographylines
- waterbodies
 - Lakes
 - Rivers



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

Notes

0 179 358 Feet

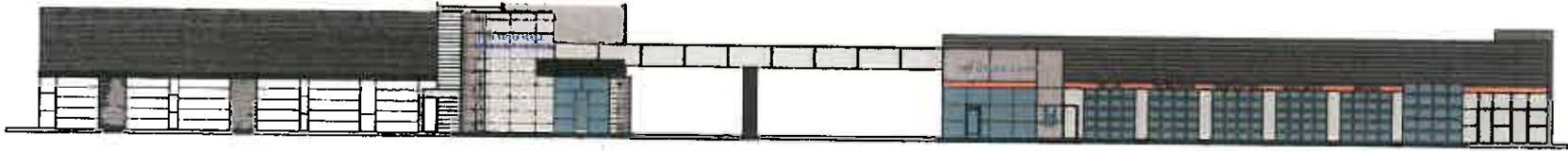


REPORT PRINTED ON... 11/2/2016 12:06:31 PM

© Riverside County RCIT GIS

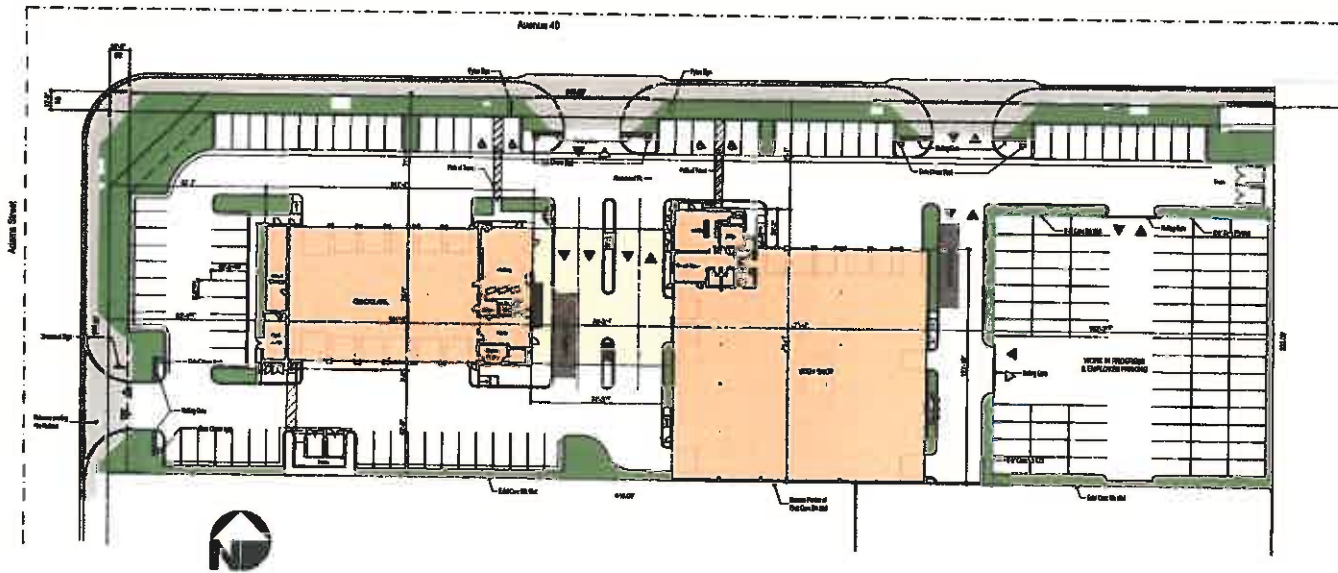
COMMERCIAL BUILDING: FIESTA FORD-INDIO QUICK LANE & I-10 AUTO MALL COLLISION CENTER

SE CORNER OF ADAMS STREET & AVENUE 40 INDIO CA, 92203

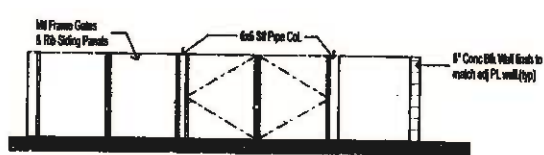


PROJECT DATA	PROJECT TEAM	SHEET INDEX																																																																																							
<p>OWNER Fiesta Ford-Indio SE Corner of Adams Street & Avenue 40 Indio CA, 92203</p> <p>LEGAL DESCRIPTION PROJECT MAP 28377 Parcels 4 & 5 CITY OF INDIO, CA.</p> <p>PROPERTY INFORMATION</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <th>LAND USE</th> <th></th> <th></th> </tr> <tr> <td>BUILDING</td> <td>28,340</td> <td>73%</td> </tr> <tr> <td>LANDSCAPE</td> <td>17,205</td> <td>10%</td> </tr> <tr> <td colspan="3">=10% of Parking Area</td> </tr> <tr> <td>PARKING</td> <td>43,750</td> <td>87%</td> </tr> <tr> <td>SITE</td> <td>124,434</td> <td>100%</td> </tr> </table> <p>PARKING</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>QUICK LANE</td> <td>10,314 SF</td> <td>53 Spaces (12 Required)</td> </tr> <tr> <td>BODY SHOP</td> <td>17,205 SF</td> <td>58 Spaces (20 Required)</td> </tr> <tr> <td colspan="3">152 Total Spaces (42 Required)</td> </tr> </table> <p>SETBACKS</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>FRONT</td> <td>20'</td> </tr> <tr> <td>REAR</td> <td>20'</td> </tr> <tr> <td>SIDE</td> <td>0'</td> </tr> </table> <p>BUILDING AREAS</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>QUICK LANE</td> <td>10,314 SF</td> </tr> <tr> <td>BODY SHOP</td> <td>17,205 SF</td> </tr> </table> <p>BUILDING STANDARDS</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>CONSTRUCTION TYPE</td> <td>V-M</td> </tr> <tr> <td>SPRINKLING</td> <td>YES</td> </tr> <tr> <td>OCCUPANCY</td> <td>B-S&I-1</td> </tr> <tr> <td>OCCUPANCY SEPARATION</td> <td>4-R</td> </tr> <tr> <td>OCCUPANCY LOAD</td> <td>1000</td> </tr> <tr> <td>MAXIMUM STORES</td> <td>3</td> </tr> <tr> <td>REQUIRED EXITS</td> <td>2</td> </tr> <tr> <td>ALLOWABLE AREA</td> <td>UNLIMITED</td> </tr> <tr> <td>MAXIMUM HEIGHT (FROM APPROVED PAD)</td> <td>30'-0"</td> </tr> </table> <p>OCCUPANCY CLASSIFICATION</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>Quick Lane</td> <td>F-1 Moderate Hazard Repair of Proceeding Automobiles & Motor Vehicles Engines</td> <td>308.2</td> <td>page 01</td> </tr> <tr> <td>Body Shop</td> <td>F-2 Low Hazard Metal Products</td> <td>308.3</td> <td>page 01</td> </tr> <tr> <td></td> <td>Not Group H</td> <td>307.1 (Exceptions)</td> <td>page 01</td> </tr> </table>	LAND USE			BUILDING	28,340	73%	LANDSCAPE	17,205	10%	=10% of Parking Area			PARKING	43,750	87%	SITE	124,434	100%	QUICK LANE	10,314 SF	53 Spaces (12 Required)	BODY SHOP	17,205 SF	58 Spaces (20 Required)	152 Total Spaces (42 Required)			FRONT	20'	REAR	20'	SIDE	0'	QUICK LANE	10,314 SF	BODY SHOP	17,205 SF	CONSTRUCTION TYPE	V-M	SPRINKLING	YES	OCCUPANCY	B-S&I-1	OCCUPANCY SEPARATION	4-R	OCCUPANCY LOAD	1000	MAXIMUM STORES	3	REQUIRED EXITS	2	ALLOWABLE AREA	UNLIMITED	MAXIMUM HEIGHT (FROM APPROVED PAD)	30'-0"	Quick Lane	F-1 Moderate Hazard Repair of Proceeding Automobiles & Motor Vehicles Engines	308.2	page 01	Body Shop	F-2 Low Hazard Metal Products	308.3	page 01		Not Group H	307.1 (Exceptions)	page 01	<p>OWNER: Fiesta Ford-Indio 7290 VANOVER ROAD INDIO CA 92203</p> <p>ARCHITECT: WALLING & MCCALLUM, LTD 45180 CLUB DRIVE INDIAN WELLS, CA 92210 JOHN WALLING, ARCHITECT 760.360.0050 walling@wmc.com</p> <p>LANDSCAPE ARCHITECT: RON GREGORY ASSOCIATES 7400 ALESSANDRO SUITE 6 PALM DESERT, CA 92260 (760.568.3634) rgreg@rpa.com</p> <p>CIVIL ENGINEER: ALLAN LEVIN ASSOCIATES 70708 BISHOP PLACE Palm Desert, CA 92260 ALLAN LEVIN 760.340.0058 alevin@levinallan.com</p> <p>APPROVAL AGENCIES: CITY OF INDIO COMMUNITY DEVELOPMENT DEPARTMENT 100 CIVIC CENTER MALL INDIO, CA 92202 (760) 951.1120</p>	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <th>SHEET</th> <th>DESCRIPTION</th> </tr> <tr> <td>0.0</td> <td>COVER SHEET</td> </tr> <tr> <td>1.0</td> <td>SCHEMATIC SITE PLAN</td> </tr> <tr> <td>2.0</td> <td>SCHEMATIC FLOOR PLAN</td> </tr> <tr> <td>3.0</td> <td>SCHEMATIC FLOOR PLAN</td> </tr> <tr> <td>4.0</td> <td>COLORS & DIMENSIONS</td> </tr> <tr> <td>5.0</td> <td>COLORS & DIMENSIONS</td> </tr> <tr> <td>6.0</td> <td>SHADING</td> </tr> <tr> <td>7.0</td> <td>LANDSCAPE PLAN</td> </tr> <tr> <td>8.0</td> <td>PLANT TRADE BOARD</td> </tr> </table>	SHEET	DESCRIPTION	0.0	COVER SHEET	1.0	SCHEMATIC SITE PLAN	2.0	SCHEMATIC FLOOR PLAN	3.0	SCHEMATIC FLOOR PLAN	4.0	COLORS & DIMENSIONS	5.0	COLORS & DIMENSIONS	6.0	SHADING	7.0	LANDSCAPE PLAN	8.0	PLANT TRADE BOARD
LAND USE																																																																																									
BUILDING	28,340	73%																																																																																							
LANDSCAPE	17,205	10%																																																																																							
=10% of Parking Area																																																																																									
PARKING	43,750	87%																																																																																							
SITE	124,434	100%																																																																																							
QUICK LANE	10,314 SF	53 Spaces (12 Required)																																																																																							
BODY SHOP	17,205 SF	58 Spaces (20 Required)																																																																																							
152 Total Spaces (42 Required)																																																																																									
FRONT	20'																																																																																								
REAR	20'																																																																																								
SIDE	0'																																																																																								
QUICK LANE	10,314 SF																																																																																								
BODY SHOP	17,205 SF																																																																																								
CONSTRUCTION TYPE	V-M																																																																																								
SPRINKLING	YES																																																																																								
OCCUPANCY	B-S&I-1																																																																																								
OCCUPANCY SEPARATION	4-R																																																																																								
OCCUPANCY LOAD	1000																																																																																								
MAXIMUM STORES	3																																																																																								
REQUIRED EXITS	2																																																																																								
ALLOWABLE AREA	UNLIMITED																																																																																								
MAXIMUM HEIGHT (FROM APPROVED PAD)	30'-0"																																																																																								
Quick Lane	F-1 Moderate Hazard Repair of Proceeding Automobiles & Motor Vehicles Engines	308.2	page 01																																																																																						
Body Shop	F-2 Low Hazard Metal Products	308.3	page 01																																																																																						
	Not Group H	307.1 (Exceptions)	page 01																																																																																						
SHEET	DESCRIPTION																																																																																								
0.0	COVER SHEET																																																																																								
1.0	SCHEMATIC SITE PLAN																																																																																								
2.0	SCHEMATIC FLOOR PLAN																																																																																								
3.0	SCHEMATIC FLOOR PLAN																																																																																								
4.0	COLORS & DIMENSIONS																																																																																								
5.0	COLORS & DIMENSIONS																																																																																								
6.0	SHADING																																																																																								
7.0	LANDSCAPE PLAN																																																																																								
8.0	PLANT TRADE BOARD																																																																																								
<p style="text-align: center;">VICINITY MAP</p>																																																																																									

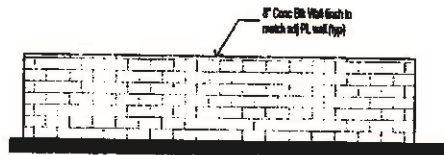
WALLING & MCCALLUM LIMITED
 COMMERCIAL BUILDING FOR
 FIESTA FORD-INDIO QUICK LANE & I-10
 AUTO MALL COLLISION CENTER
 100 CIVIC CENTER MALL
 INDIO, CA 92202
 COVER SHEET
 1.0
 SCALE: 1/8" = 1'-0"
 DATE: 10/20/2010
 DRAWN BY: J. WALLING
 CHECKED BY: J. WALLING
 PROJECT NO: 10-001



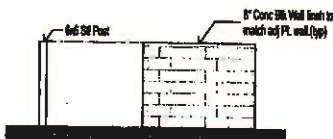
SCHEMATIC SITE PLAN
SCALE: 1" = 30'



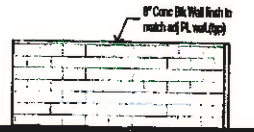
A FRONT ELEVATION
SCALE: 1/4" = 1'-0"



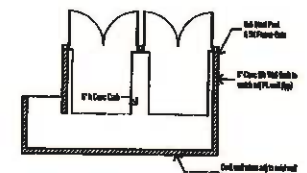
C REAR ELEVATION
SCALE: 1/4" = 1'-0"



B RIGHT SIDE ELEVATION
SCALE: 1/4" = 1'-0"



D LEFT SIDE ELEVATION
SCALE: 1/4" = 1'-0"



TRASH ENCLOSURE FLOOR PLAN
SCALE: 1/4" = 1'-0"

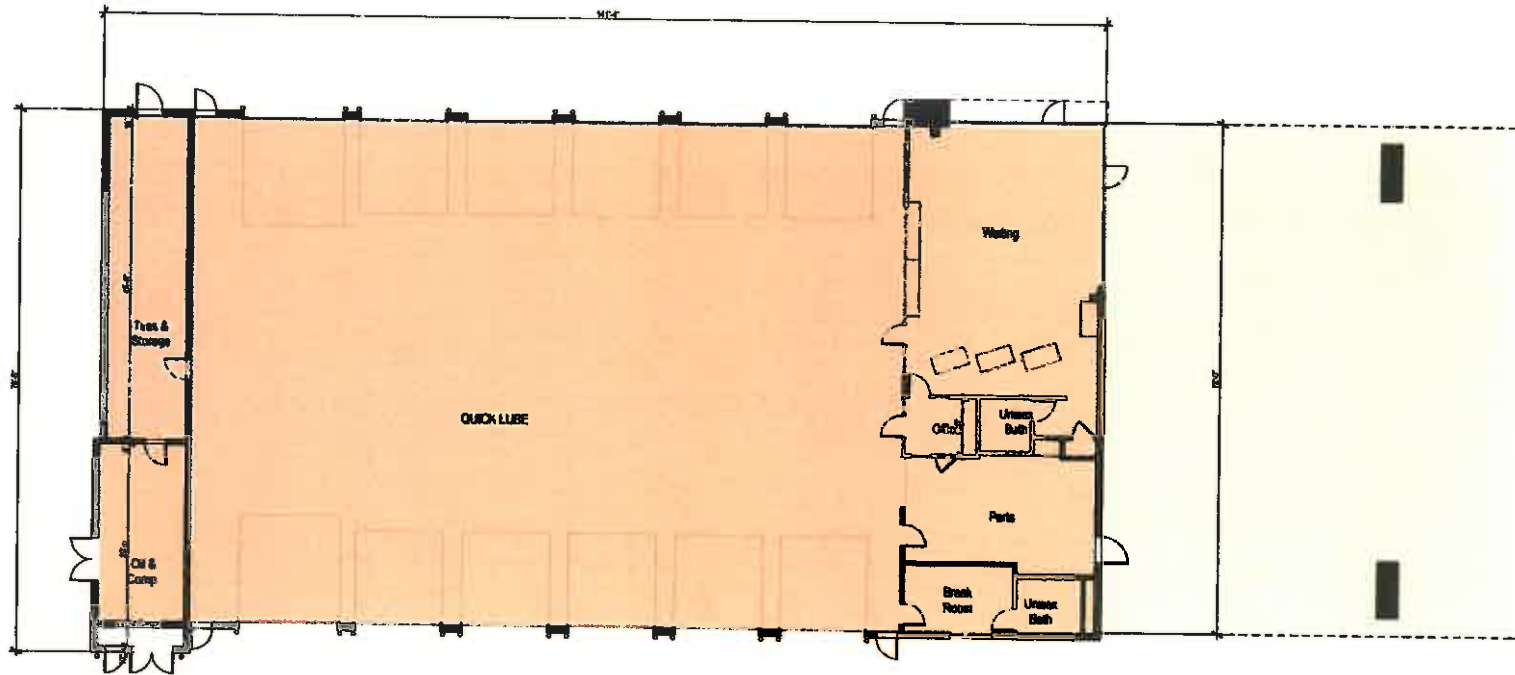
ARCHITECTURAL ENGINEERING & PLANNING
WALLING & MCALLUM LIMITED

SCHEMATIC SITE PLAN

PIESTA FORD-INDIO QUICK LANE & I-10
AUTO MALL COLLISION CENTER

2.0

DATE: 10/15/16



SCHEMATIC FLOOR PLAN
SCALE: 1/8" = 1'-0"

ARCHITECTURAL FLOOR PLANS & INTERIORS
WALLING & MCCALLUM LIMITED
1000 WEST 10TH AVENUE, SUITE 100, DENVER, CO 80202
TEL: 303.733.1111

SCHEMATIC FLOOR PLANS

3.0

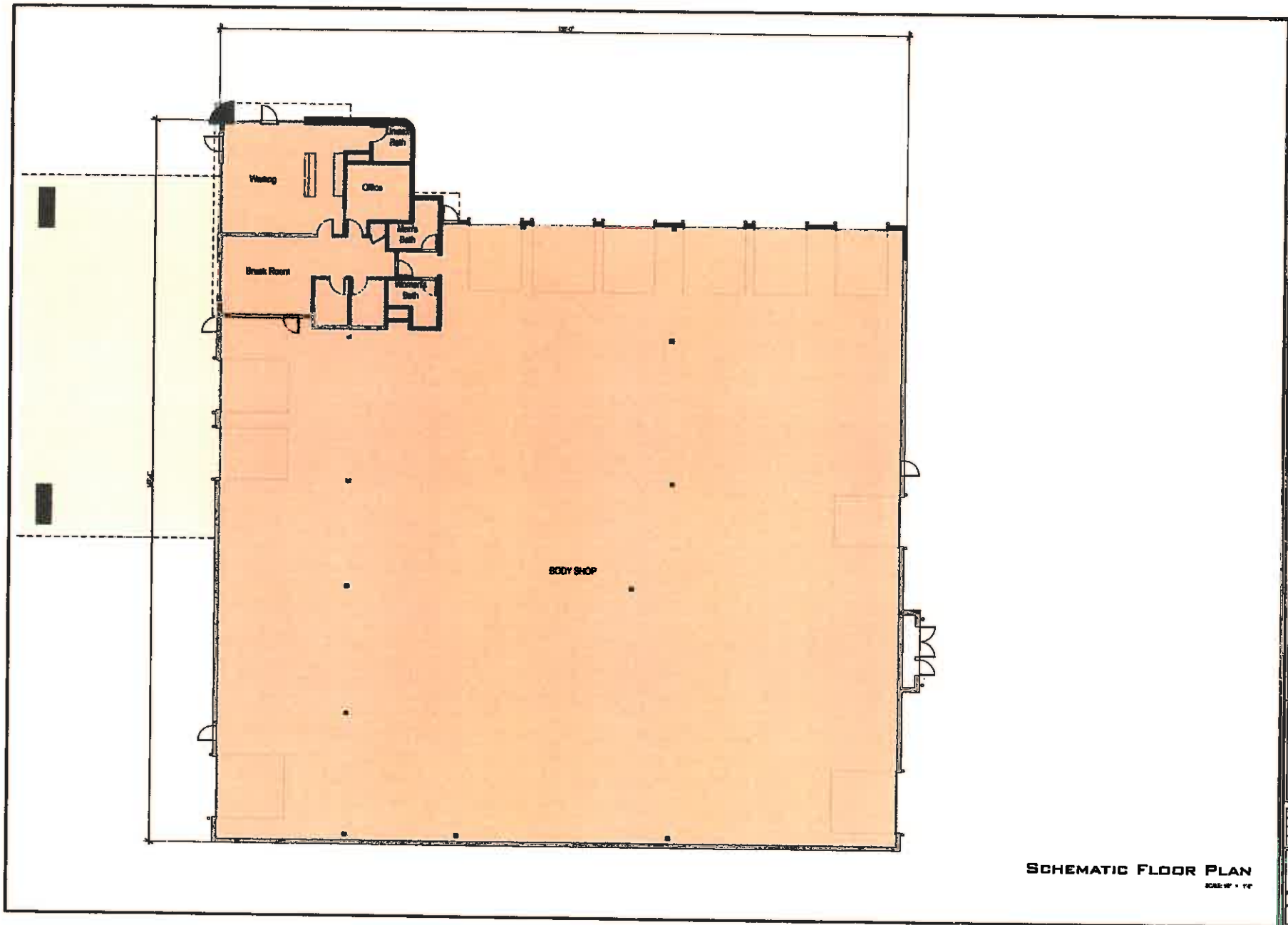
REVISED

NOV 15 2011

10/25/11

10/25/11

10/25/11



SCHEMATIC FLOOR PLAN
SCALE: 1/4" = 1'-0"

COMMERCIAL BUILDING FROM
PRESTA FORD-INDIO QUICK LANE & I-10
AUTO MALL COLLISION CENTER
 25000 N. ALHAMBRA BLVD., ALHAMBRA, CA 91801
 626-281-1111

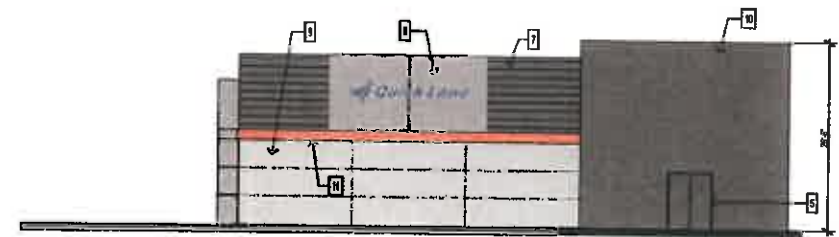
ARCHITECTURAL FIRM: WALLING & McALLUM LIMITED
 10000 N. ALHAMBRA BLVD., SUITE 100, ALHAMBRA, CA 91801
 626-281-1111

SCHEMATIC FLOOR PLAN
 3.1

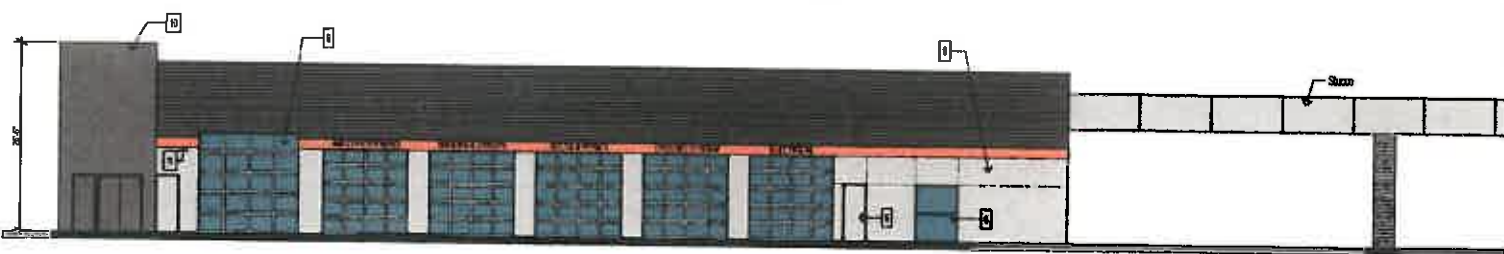
10/27/18
 WALLING & McALLUM LIMITED
 ARCHITECTS



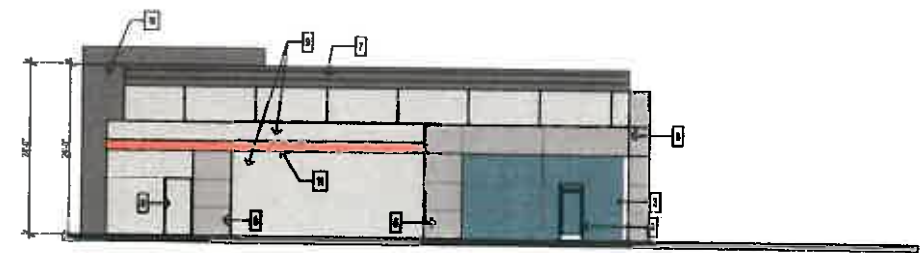
A AVE 40 STREET ELEVATION
SCALE: 1/4" = 1'-0"



B ADAMS AVE ELEVATION
SCALE: 1/4" = 1'-0"



C REAR ELEVATION
SCALE: 1/4" = 1'-0"



D LEFT SIDE ELEVATION
SCALE: 1/4" = 1'-0"

- ELEVATION GENERAL NOTES**
- A. REFER TO FLOOR PLANS FOR DIMENSION NOT SHOWN.
 - B. REFER TO STRUCTURAL PLANS FOR SIZES & DETAILS OF ALL FOOTINGS.
 - C. REFER FOUNDATION PLAN FOR SIZES AND DETAILS OF ALL FOOTINGS.
 - D. REFER TO WINDOW & DOOR SCHEDULES FOR MORE INFORMATION.
 - E. ALL GLAZING IN HAZARDOUS LOCATIONS AS DEFINED BY 2007 CBC 2404.1 SHALL BE TEMPERED GLASS. SEE NOTES SHEETS 10 - 18 - TEMPERED GLASS, 1 LAM - LAMINATE GLASS.
 - F. EXTERIOR FINISH SHALL BE 5-COM. 7/8" PLASTER
 - a. CURTIN COLOR PER EXTERIOR MATERIALS LEGEND, THIS SHEET.
 - b. SUBST. IN GENERAL NOTES FOR APPLICATION REQUIREMENTS.
 - c. CONVENTIONAL 7/8" STUCCO OVER DUPONT TYVEE STUCCO (OR LAMINATE) (S-375).
 - d. SCRATCH AND BROWN COATS TO INCORPORATE SHAPED FIBERS.
 - G. PROVIDE 1/2" SPAN ANGLESTEEL AS SHOWN 1/4" ABOVE THE VALLEYS AND 1/2" BELOW 1/4" OF THE CORNER.
 - H. ALL FINISHES SHOWN ARE TO BE FINISHED FROM TO SLAB (4" MIN.) TO TOP OF SLAB (4" MIN.) ABOVE APPROVED GRADE.
 - I. ALL EXTERIOR OPENINGS SHALL BE FLASHED AND COUNTER FLASHED WEATHERSTRIP ALL EXTERIOR DOORS PER CBC 1402.
 - J. GENERAL CONTRACTOR SHALL BE RESPONSIBLE TO INSURE A WATER-TIGHT SEAL AT ALL EXTERIOR FINISHES.
 - K.

- ELEVATION REFERENCE NOTES**
1. PLASTER FINISH (TYPE)
 - a. SCRATCH "SMALL MARBLE" FINISH
 - b. SCRATCH CONCRETE TYPE
 - c. PROVIDE A WEEP SCREED (PER IBC) AT ALL EXTERIOR PLASTER TO FOUNDATION CONNECTIONS. WEEP SCREED AT FOUNDATION I USE ELASTOMERIC JOINT.
 - d. SEE GENERAL NOTES ABOVE FOR MORE FINISH AND INSTALLATION INFORMATION.
 2. STUCCO - SEE SIDEWALK SHEET S-3
 3. CLEAR ALUM FRAME STOREFRONT GLASS SYSTEM OF PPG ALLIE SUPRIRE GLASS TYPICAL.
 4. CLEAR ALUM FRAME STOREFRONT GLASS DOOR IN PPG BLUE SHIPWRECK GLASS TYPICAL.
 5. HOLLOW METAL DOOR
 6. OVERHEAD DOOR WEAR INSERTS
 7. UPPER HORIZONTAL SILING "Double (Glass Reinforced) 7/8" Ins. (Dry) Glass Only"
 8. ENTRY FRACCIA & SCHWABERS STR. SIGN BACKGROUND "White" ACM (Medium) (S-375)
 9. SMOOTH PLASTER FINISH BROWN, GRAY OR BLUE W/ PLASTER CONTROL JOINTS.
 10. SMOOTH PLASTER FINISH GABLETILE GRAY SPP7219
 11. SHEET METAL SAND TANGS SMO648
 12. MODERN MASTERS BELVIN WE180

ARCHITECTURE • INTERIOR DESIGN • PLUMBING

WALLING & MOCALLUM LIMITED

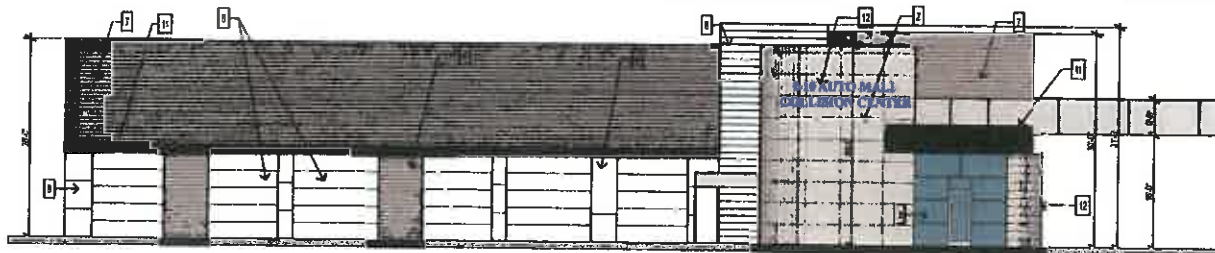
COMMERCIAL BUILDINGS FROM
PISTA FORD-INDIO QUICK LANE & I-10
AUTO MALL COLLISION CENTER
300 NORTH OF ADAMS STREET & AVENUE 40
MOUNTAIN VIEW, COLORADO 80901

COLORED
ELEVATIONS

RESOLUTION

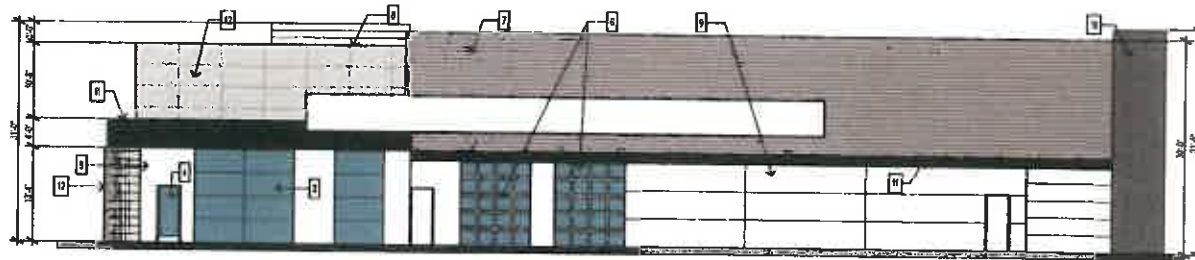
4.0

DATE: 10-1-18
PROJECT: 18-001
SHEET: 01-01



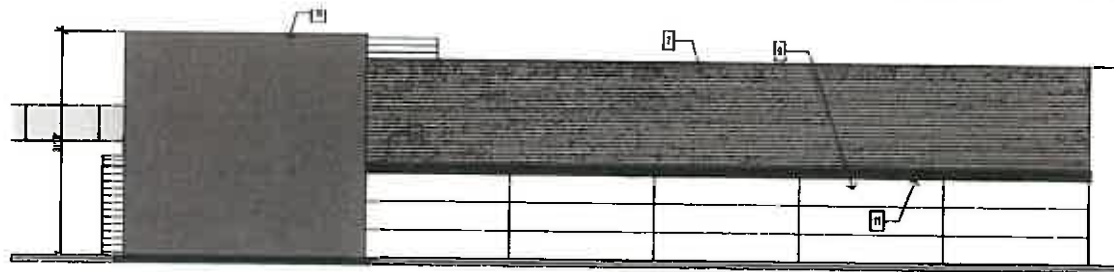
A AVE 40 STREET ELEVATION

SCALE 1/8" = 1'-0"



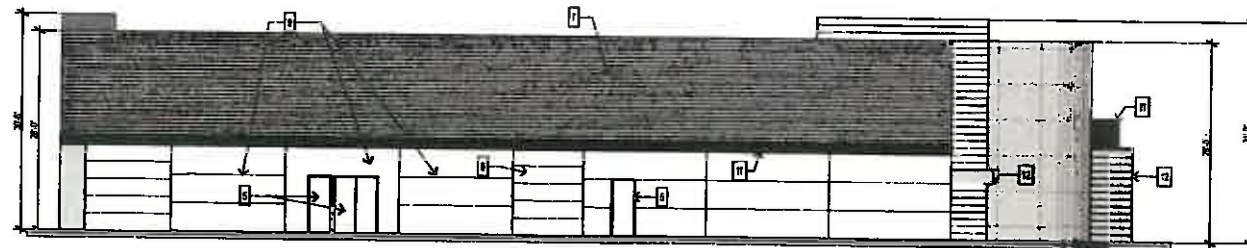
B RIGHT SIDE ELEVATION

SCALE 1/8" = 1'-0"



C REAR ELEVATION

SCALE 1/8" = 1'-0"



D LEFT SIDE ELEVATION

SCALE 1/8" = 1'-0"

ELEVATION GENERAL NOTES

- A REFER TO FLOOR PLANS FOR DIMENSIONS NOT SHOWN
- B REFER TO STRUCTURAL PLANS FOR SIZES & DETAILS OF ALL STRUCTURAL MEMBERS AND DIMENSIONS
- C REFER TO FOUNDATION PLAN FOR SIZES AND DETAILS OF ALL FOOTINGS
- D REFER TO WINDOW & DOOR SCHEDULES FOR MORE INFORMATION
- E ALL GLAZING IN HEADSPACE LOCATIONS AS DEFINED IN 2001 CBC 2401.1 SHALL BE TEMPERED GLASS - SEE NOTES SHEETS D, H & I
 # TG = TEMPERED GLASS, # LAM = LAMINATE GLASS
- F EXTERIOR FINISH SHALL BE 3-COAT 7/8" PLASTER
 A. CUSTOM COLOR PER EXTERIOR MATERIALS LOGBOOK, THIS SHEET
 B. SHEET 18 GENERAL NOTES FOR APPLICATION REGISTRATION
 C. CONVENTIONAL 7/8" STUCCO OVER SUPPORT TYPE STUCCO WRAP (2018-2019)
 D. SCRATCH AND BROWN COATS TO INCORPORATE CHOPPED FIBER
- G PROVIDE 1/8" BRASS ANGLESTOPS AS SHOWN 2" ABOVE THE TALLEST ROCK WITHIN 18" OF THE CORNER
- H ALL HEIGHTS SHOWN ARE TO T-FRAMING FROM T-SLAB (10'-0") TOP OF SLAB @ 4" IS 0" ABOVE APPROVED GRADE
- I ALL EXTERIOR OPENINGS SHALL BE FLASHED AND COUNTER FLASHED WEATHERSTRIP ALL EXTERIOR DOORS PER CBC 1402
- J GENERAL CONTRACTOR SHALL BE RESPONSIBLE TO PROVIDE A WATER-TIGHT SEAL AT ALL EXTERIOR FINISHES

ELEVATION REFERENCE NOTES

- A PLASTER FINISH (P/F)
- B SMOOTH "SANTA BARBARA" FINISH
- C SCHEDULED CORNER (C/C)
- D PROVIDE A WEEP SCREED (2018 MIN.) AT ALL EXTERIOR PLASTER TO FOUNDATION CONNECTIONS. WEEP SCREED AT FOUNDATION LINE ELASTOMERIC JOINT
- E SEE GENERAL NOTES ABOVE FOR MORE FINISH AND INSTALLATION INFORMATION
- F SEE DETAILS FOR MORE INFORMATION
- G PROVIDE SAMPLE FOR APPROVAL PRIOR TO APPLICATION
- H SCHEDULE - SEE SCHEDULE SHEET S-5
- I CLEAR ALUM. FRAME STOREFRONT GLASS SYSTEM w/ PPG BLUE SAFIRAZ GLASS TYPICAL
- J CLEAR ALUM. FRAME STOREFRONT GLASS DOOR w/ PPG BLUE SAFIRAZ GLASS TYPICAL
- K HOLLOW METAL DOOR
- L OVERHEAD DOOR W/GLASS INSERTS
- M UPPER HORIZONTAL SIDING
 Superior Cedar Siding 7/2" No Siding Color Gray
- N ENTRY TOWER
 Siding: (Atom Aluminum) 1/2" Bob Siding Pure White
- O STUCCO WALLS
 SMOOTH PLASTER FINISH MODERN MASTERS
 SHOWPLACE METRIC IN PLASTER CONTROL JOINTS
- P REAR TOWER
 SMOOTH PLASTER FINISH GAUNTLET GRAY SW2014
- Q FASCIA & BAND
 ALCOA REYNOLDS MODERN MASTERS STEEL GRAY M224
- R CURVED WALL @ ENTRY
 SMOOTH PLASTER FINISH MODERN MASTERS SILVER M214

ARCHITECTURE • ENGINEERING • INTERIORS
 WALLING & MCCALLUM LIMITED
 1000 WEST 10TH AVENUE, SUITE 200, DENVER, CO 80202
 TEL: 303.733.8888 FAX: 303.733.8889
 WWW.WALLINGANDMCCALLUM.COM

CONVENTIONAL BUILDING FROM
 PIESTA FORD-INDIC QUICK LANE & I-10
 AUTO MALL COLLISION CENTER
 41 ELEVATIONS
 10.5.16
 4.1

Man Power Utilization

AREA

Quick Lane 12 stall Auto Care

1	1 Manager	Office	100	sq. ft
2	3 Service Writers	Customer Service Area	1026	sq. ft
3	1 Parts Person	Parts Room	405	sq. ft
4	8 Auto Technicians	Service Bays	5800	sq. ft
13	Total			
	Storage		2742	sq. ft
	Bath rooms, Break room, etc.		244	sq. ft
			<hr/>	
			10317	sq. ft

I 10 Auto Collision Ctr

1	1 Manager	Office	144	sq. ft
2	1 Assistant Manager	Lobby	484	sq. ft
3	10 Auto Body Technicians	Service Bays	12125	sq. ft
12	Total			
	Storage, Break Room, etc.		4684	sq. ft
	Bathrooms, locker rooms, etc.		577	sq. ft
			<hr/>	
			18014	sq. ft

Man Power Utilization

AREA

Quick Lane 12 stall Auto Care

1	1 Manager	Office
2	3 Service Writers	Customer Service Area
3	1 Parts Person	Parts Room
4	<u>8</u> Auto Technicians	Service Bays
13	Total	

I 10 Auto Collision Ctr

1	1 Manager	Office
2	1 Assistant Manager	Lobby
3	<u>10</u> Auto Body Technicians	Service Bays
12	Total	

NOTICE OF PUBLIC HEARING

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Thursday from 8:00 a.m. to 5:00 p.m., except Thursday, November 24 (Thanksgiving Day), and by prescheduled appointment on Friday, December 2, from 8:30 a.m. to 5:00 p.m. Please send any written comments to this address.

ATTENTION: ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. The City of Indio may hold hearings on this project and should be contacted on non-ALUC issues.

PLACE OF HEARING: Eastern Municipal Water District
2270 Trumble Road, Board Chambers
Perris, California
[Hearing venue only – do not send written comments to this address]

DATE OF HEARING: December 8, 2016

TIME OF HEARING: 9:00 A.M.

CASE DESCRIPTION:

ZAP1066BD16 – Fiesta Ford, Inc. (Representative: KSC, Inc.) – City of Indio Planning Case Nos. 16-10-403 (Design Review) and 16-10-1018 (Conditional Use Permit). The applicant is proposing to develop a Fiesta Ford Quick Lane and Collision Center auto repair facility consisting of a 10,314 square foot service repair building and 17,826 square foot collision building on 2.8 acres (Assessor's Parcel Numbers 607-230-038 and 607-230-040). The project site is located on the southeast corner of Avenue 40 and Adams Street in the City of Indio (Airport Compatibility Zone B1, C and D of the Bermuda Dunes Airport Influence Area).

FURTHER INFORMATION: Contact Paul Rull at (951) 955-6893. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Ms. Leila Namvar of the City of Indio Planning Department at (760) 541-4258.

APPLICATION FOR MAJOR LAND USE ACTION REVIEW
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

ALUC Identification No.

ZAP 1066BD16

PROJECT PROPONENT (TO BE COMPLETED BY APPLICANT)

Date of Application 10/11/2016
 Property Owner Fiesta Ford, Inc. Phone Number 760 772 8000
 Mailing Address 79880 Varner Rd. Indio, CA 92203

Agent (if any) Ken Stendell, KSC Inc. Phone Number 760 578 1918
 Mailing Address P O Box 3352 Palm Desert, CA 92261
ksc.inc@hotmail.com

PROJECT LOCATION (TO BE COMPLETED BY APPLICANT)

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address SE corner of Adams & Avenue 40
 Assessor's Parcel No. 607-230-038 & 040 Parcel Size 2.85 ac
 Subdivision Name _____ Zoning Classification BP
 Lot Number _____

PROJECT DESCRIPTION (TO BE COMPLETED BY APPLICANT)

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe) (D) Light industrial including, but not limited to, auto body shop, paint shop, product service center or engine rebuilding shop.

Proposed Land Use (describe) Annex facility for the existing Fiesta Ford Automotive Dealership to allow them to expand two automotive important services.
Also allow for additional employee parking as spaces exceed the required

For Residential Uses Number of Parcels or Units on Site (exclude secondary units) _____
 For Other Land Uses Hours of Use 7:00 am - 6:00 pm Monday thru Saturday
 (See Appendix C) Number of People on Site Maximum Number 25
 Method of Calculation Number of working positions

Height Data Height above Ground or Tallest Object (including antennas and trees) _____ 31 ft.
 Highest Elevation (above sea level) of Any Object or Terrain on Site _____ 121.1ft.

Flight Hazards Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight? Yes
 No
 If yes, describe _____

B. Jones
 by C, D

REFERRING AGENCY (APPLICANT OR JURISDICTION TO COMPLETE)	
Date Received	Type of Project
Agency Name: <u>City of Indio</u>	<input type="checkbox"/> General Plan Amendment
Staff Contact: <u>Leila Namvar</u>	<input type="checkbox"/> Zoning Amendment or Variance
Phone Number	<input type="checkbox"/> Subdivision Approval
Agency's Project No.: <u>Design Review 16-10-403</u> <u>CUP 16-10-1018</u>	<input type="checkbox"/> Use Permit
	<input type="checkbox"/> Public Facility
	<input type="checkbox"/> Other

A. **NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. **SUBMISSION PACKAGE:**

ALUC REVIEW

STAFF REVIEW (Consult with ALUC staff planner as to whether project qualifies)

- 1 Completed Application Form
- 1 Project Site Plan – Folded (8-1/2 x 14 max.)
- 1 Elevations of Buildings - Folded
- 1 Each . 8 ½ x 11 reduced copy of the above
- 1 8 ½ x 11 reduced copy showing project in relationship to airport.
- 1 Set . Floor plans for non-residential projects
- 4 Sets. Gummed address labels of the Owner and representative (*See Proponent*).
- 1 Set . Gummed address labels of all property owners within a 300' radius of the project site. If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10), with ALUC return address.
- 4 Sets. Gummed address labels of the referring agency (City or County).
- 1 Check for Fee (See Item "C" below)

- 1 Completed Application Form
- 1 Project Site Plans – Folded (8-1/2 x 14 max.)
- 1 Elevations of Buildings - Folded
- 1 8 ½ x 11 Vicinity Map
- 1 Set . Gummed address labels of the Owner and representative (*See Proponent*).
- 1 Set . Gummed address labels of the referring agency.
- 1 Check for review–See Below

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.5

HEARING DATE: December 8, 2016

CASE NUMBER: ZAP1067BD16 – Jefferson Development 16, LLC.
(Representative: William Sharon, McGee Sharon Architects,
Inc.)

APPROVING JURISDICTION: City of Indio

JURISDICTION CASE NO: CUP16-10-1019 (Conditional Use Permit), DR16-10-404
(Design Review)

MAJOR ISSUES: The proposed project exceeds the average acre and single acre intensity criteria for Compatibility Zone B1 with occupancy calculated using the Building Code Method or Parking Space method. Also, the project does not provide the required 30% open area for emergency landing open area purposes.

RECOMMENDATION: Staff recommends that the Commission find the Conditional Use Permit and Design Review **INCONSISTENT**, based on the proposed project exceeding average and single acre intensities and not providing the required 30% acreage for open area. Given that the site is located in Airport Compatibility Zone B1, we suggest that the proposed land uses, other than the self-storage facility, be re-evaluated. An office/warehousing facility would likely meet intensity limits.

PROJECT DESCRIPTION: A proposal to develop a commercial center with retail and restaurant buildings, convenience store with fuel pumps, and self-storage facility on 19.26 acres. The project includes 50,300 square feet in five retail buildings (including a 16,200 square foot building suitable for a chain drug store pharmacy), five freestanding pads along the frontage (four with drive-thru lanes) with a cumulative floor area of 21,000 square feet, a 4,300 square foot convenience store with deli, ten gasoline pumps (20 service locations), and automated drive-thru car wash, and a seven-building 81,915 square foot self-storage facility with on-site caretaker's residence

PROJECT LOCATION: The site is located along the northerly side of Indio Boulevard, easterly of its overcrossing of Interstate 10, southerly of the rail line and freeway, and westerly of Madio and Madison Streets in the City of Indio, approximately 1,500 feet easterly of runway 10-28 at Bermuda Dunes Airport.

LAND USE PLAN: 2004 Bermuda Dunes Airport Land Use Compatibility Plan

- a. Airport Influence Area: Bermuda Dunes Airport
- b. Land Use Policy: Compatibility Zones B1 and A
- c. Noise Levels: 60 – 65+ CNEL contour

BACKGROUND:

ZAP1015BD07: The Commission had previously found the original proposal for 17 industrial buildings with a total gross floor area of 159,790 square feet on 16.65 net acres on this parcel consistent via ZAP1015BD07. The original project provided for a total of 66,452 square feet of office space and 99,678 square feet of warehouse area, accommodating an average intensity of 25 people per acre and 52 people per single acre intensity, which were found consistent with Compatibility Zone B1 criterion (with the use of risk-reduction design features used in calculating single acre intensity).

Non-Residential Average Intensity: Pursuant to the 2004 Bermuda Dunes Airport Land Use Compatibility Plan, the project site is located within Compatibility Zones B1 and A, although the project does not propose any development in the portion located in Zone A. Compatibility Zone B1 restricts average intensity to 25 people per acre. The site includes 1.34 acres in Zone A. This portion of the site has no allowable intensity and is excluded from intensity calculations.

The “Building Code Method” for calculating intensity utilizes “minimum floor area per occupant” criteria from the Building Code as a factor in projecting intensity. Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, the following intensities were utilized for the project:

- office areas – 1 person/200 square feet;
- retail – 1 person/60 square feet;
- restaurant – 1 person/15 square feet;
- storage areas – 1 person/300 square feet; and
- applicant trip counts anticipate a maximum of 50 daily visitors to the mini-storage facility based on daily visitor trips.

The overall project includes an 81,915 square foot mini-storage facility, 4,300 square foot convenience store/gas station facility with car wash tunnel, 50,300 square feet of retail building area, and 21,000 square feet of restaurant drive-thru building area. The total building area onsite is 157,515 square feet, resulting in an overall occupancy of 2,583 people (273 people for mini-storage facility, 72 people for the gas station facility, 838 people for the retail building area, 1,400 people for the restaurant area), and an average intensity of 144 people per acre, which is inconsistent with Compatibility Zone B1 average intensity criteria of 25 people per acre.

If we were to consider the applicant's mini-storage facility daily visitor trip counts of 50 visitors instead of the 273 people (1 person per 300 square feet of storage area) as calculated per the Building Code, the total occupancy of the site would be 2,360 people, and an average intensity of 132 people per acre, which is still inconsistent with Compatibility Zone B1 average intensity criteria of 25 people per acre.

If the project site was calculated using the applicant's 50 visitors mini-storage facility count with an assumption that half of the restaurant area would be kitchen area rather than dining area, the number of people in the restaurants would be reduced to 753, for a total occupancy of 1,713 people, for an average intensity of 96 persons per acre, which is still inconsistent.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per standard vehicle). Based on the number of standard parking spaces provided of 575 spaces, the total occupancy for the entire site would be 863 people, which results in an overall average intensity of 48 persons per acre, which is also inconsistent with Compatibility Zone B1 average intensity criteria of 25 persons per acre.

Non-Residential Single-Acre Intensity: Pursuant to the 2004 Bermuda Dunes Airport Land Use Compatibility Plan, the project site is located within Compatibility Zone A and B1, although the project does not propose any development in the portions located in Zone A. Zone B1 restricts single acre intensity to 50 people per acre.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre area occurs at Building B which includes 4,800 square feet of restaurant area, resulting in an occupancy of 320 people which is inconsistent with Compatibility Zone B1 single-acre criterion of 50. Even if half of the restaurant is kitchen area, the restaurant would accommodate 172 people.

Another area of the site that could potentially have a high occupancy is Building H which includes 16,200 square feet of retail area, resulting in an occupancy of 270 people which is inconsistent with Compatibility Zone B1 single-acre criterion of 50.

Prohibited and Discouraged Uses: The applicant does not propose any development in the portions located in Compatibility Zone A, and does not propose any uses specifically prohibited or discouraged in Compatibility Zone B1 of the Bermuda Dunes Airport Influence Area. However, the intensity of the proposed uses clearly exceeds Compatibility Zone B1 limits.

Noise: The site is located within the 60 - 65+ CNEL contour range from aircraft noise. As a primarily commercial use not sensitive to noise, the commercial retail center would not require special measures to mitigate aircraft-generated noise.

Part 77: The elevation of Runway 10-28 at its westerly terminus is approximately 49 feet above mean sea level (AMSL). At a distance of approximately 1,500 feet from the runway, FAA review would be required for any structures with top of roof exceeding 64 feet AMSL. The project's site elevation is 40 feet AMSL, and the height of the building is 32 feet, for a maximum top point elevation of 72 feet AMSL. Therefore, Federal Aviation Administration (FAA) obstruction evaluation review for height/elevation reasons is required. The applicant has submitted Form 7460-1, and FAAOES has assigned Aeronautical Study No. 2016-AWP-10990 thru 10995-OE to this project.

Open Area: The site is located within Airport Compatibility Zones A and B1, although the project does not propose any development in the portions located in Zone A. Compatibility Zone B1 requires projects 10 acres or larger to designate 30% of project area as ALUC-qualifying open area that could potentially serve as emergency landing areas. Based on the size of the B1 area (17.92 acres), the project is required to provide a minimum of 5.38 acres of open area consistent with ALUC open area criteria. The proposed project does not specify any "set aside" of ALUC eligible open areas in Zone B1, although it should be acknowledged that the westerly 1.34 acres in Zone A would remain open.

CONDITIONS:

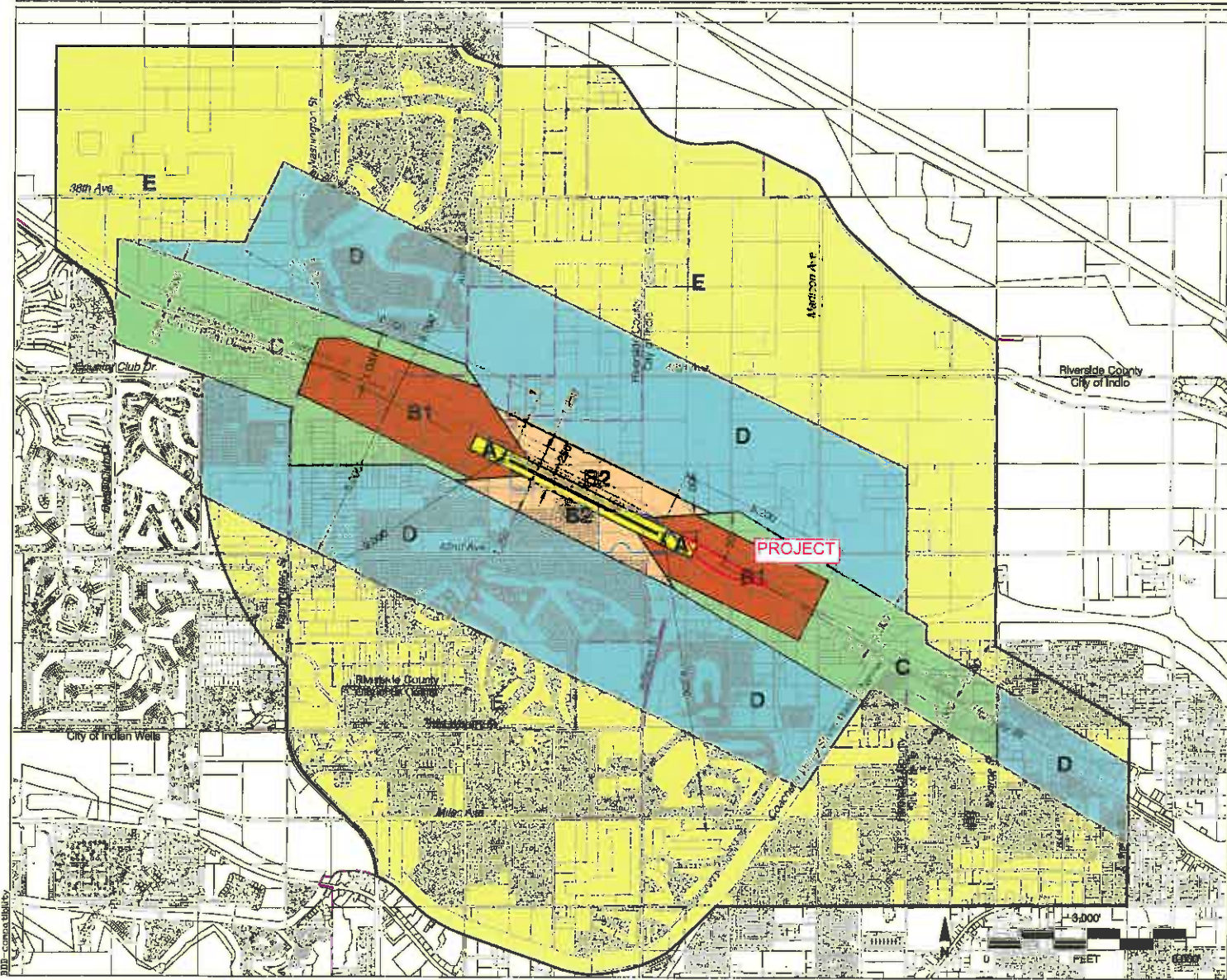
1. Any outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. All outdoor lighting plans shall be subject to review by airport management.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use or activity which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use or activity which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use or activity which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, composting operations, production of cereal grains, sunflower, and row crops, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)

- (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, day care centers, libraries, hospitals, nursing homes, highly noise-sensitive outdoor nonresidential uses, places of worship, aboveground bulk storage of 6,000 gallons or more of flammable or hazardous materials, and hazards to flight.
3. The attached notice shall be given to all prospective purchasers and/or tenants of the property.
 4. Prior to issuance of a building permit, the property owner shall convey an avigation easement to Bermuda Dunes Airport. Copies of the recorded avigation easement shall be forwarded to the Airport Land Use Commission and to the City of Indio.
 5. Any new detention basin(s) on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.

Trees shall be spaced to avoid creation of a continuous canopy. Landscaping in and around the detention basin shall not include vegetation that produces seeds, fruits, or berries.
 6. Any subsequent Design Review, Conditional Use Permit, Tenant Improvement, or other permitting that would alter the use and occupancy of the currently proposed project shall require ALUC review.
 7. The applicant shall submit plans for the proposed gas station fueling tanks with the Riverside County Department of Environmental Health Hazardous Materials Branch for review and approval according to their safety regulations and such mitigations shall be in place to protect the public safety in the event that an aircraft ruptures and ignites the fueling pumps and fueling tanks.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Legend

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E

Boundary Lines

- Airport Property Line
- City Limits

Note

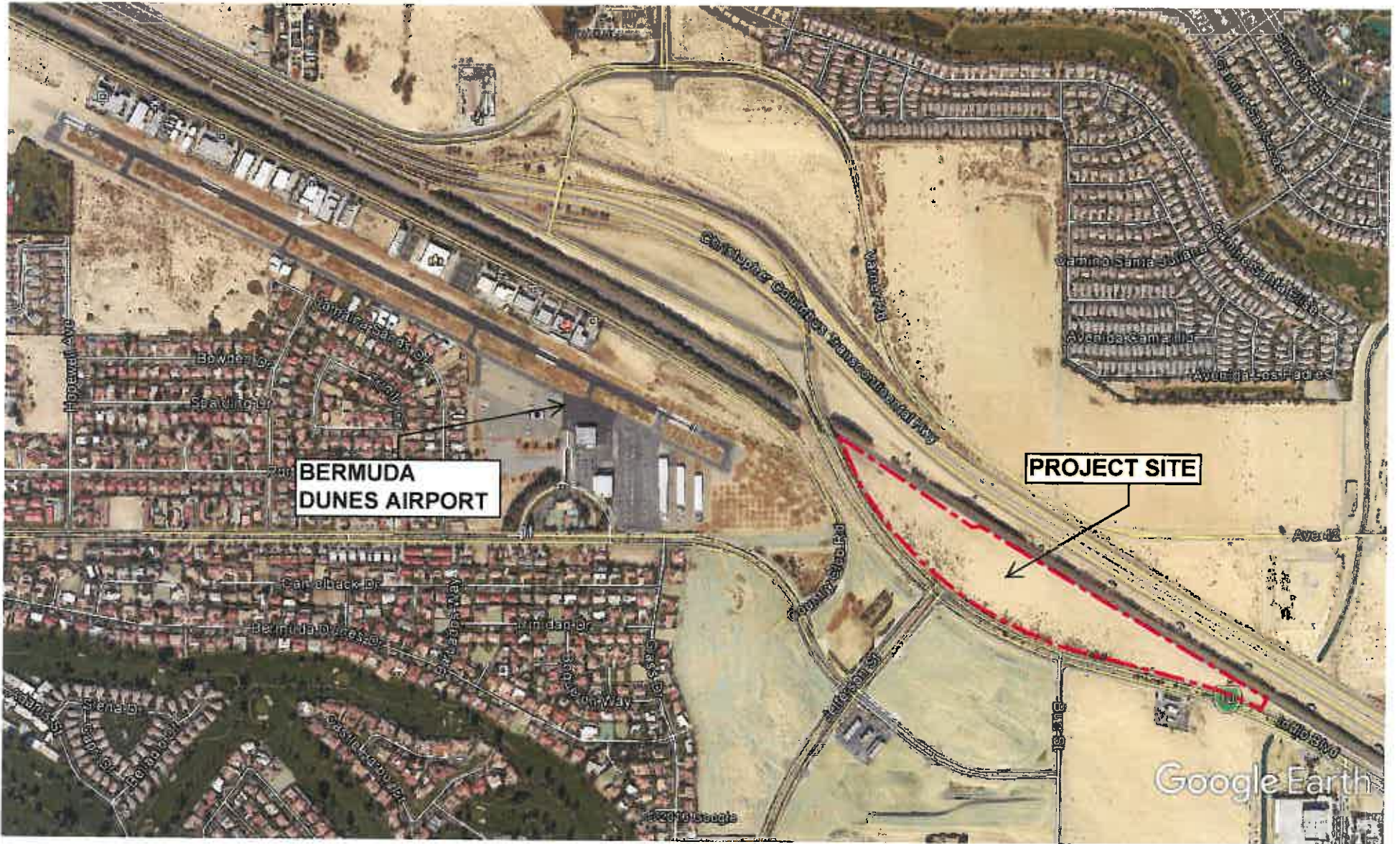
Southwestern edge of Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

See Chapter 2, Table 2A for compatibility criteria associated with this map.

Riverside County
 Airport Land Use Commission
**Riverside County
 Airport Land Use Compatibility Plan
 Policy Document**
 (Adopted December 2004)

Map BD-1

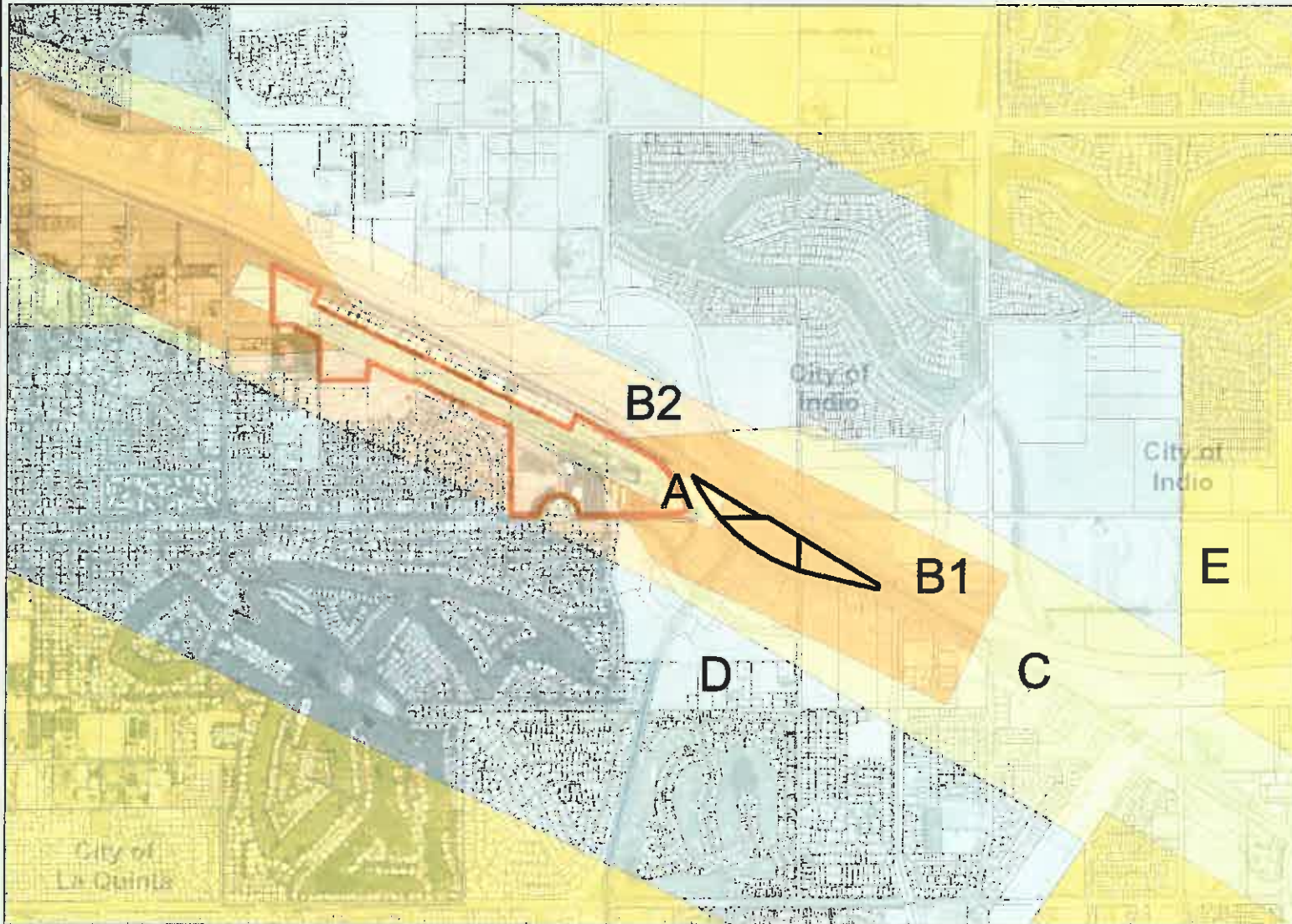
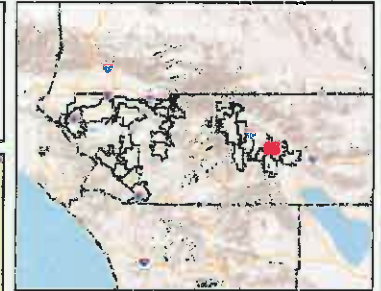
Compatibility Map
 Bermuda Dunes Airport



JEFFERSON STREET PROJECT

INDIO, CALIFORNIA

My Map



Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ II-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

Notes

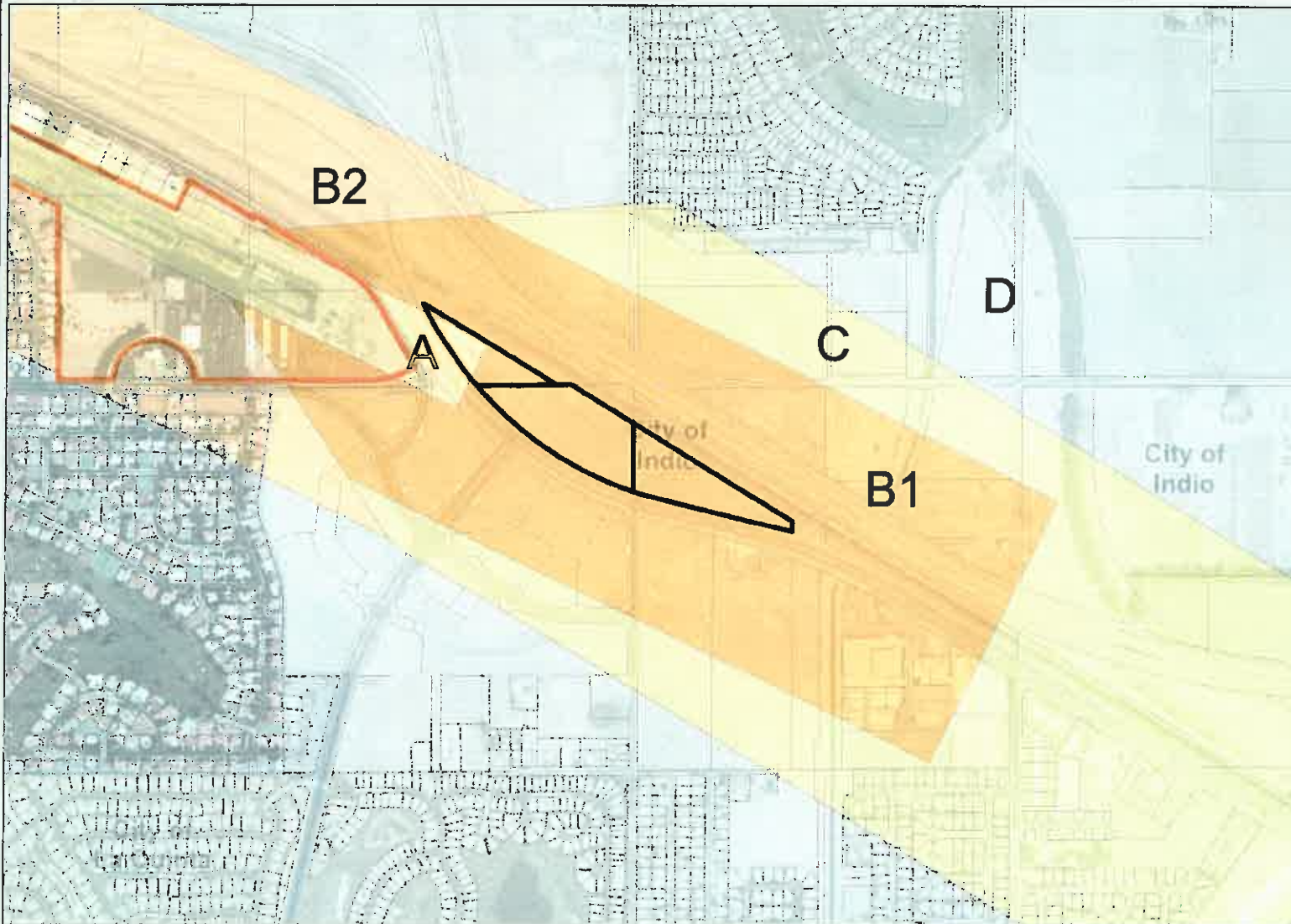
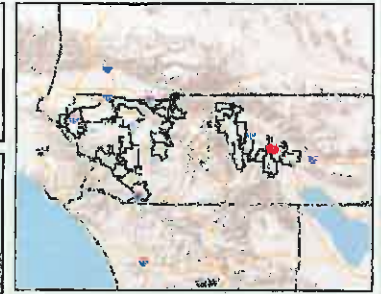
0 2,568 5,136 Feet



REPORT PRINTED ON... 11/10/2016 1:12:02 PM

© Riverside County RCIT GIS

My Map



Legend

- Display Parcels
- Airports
- AIA

Airport Compatibility

- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



0 1,284 2,568 Feet



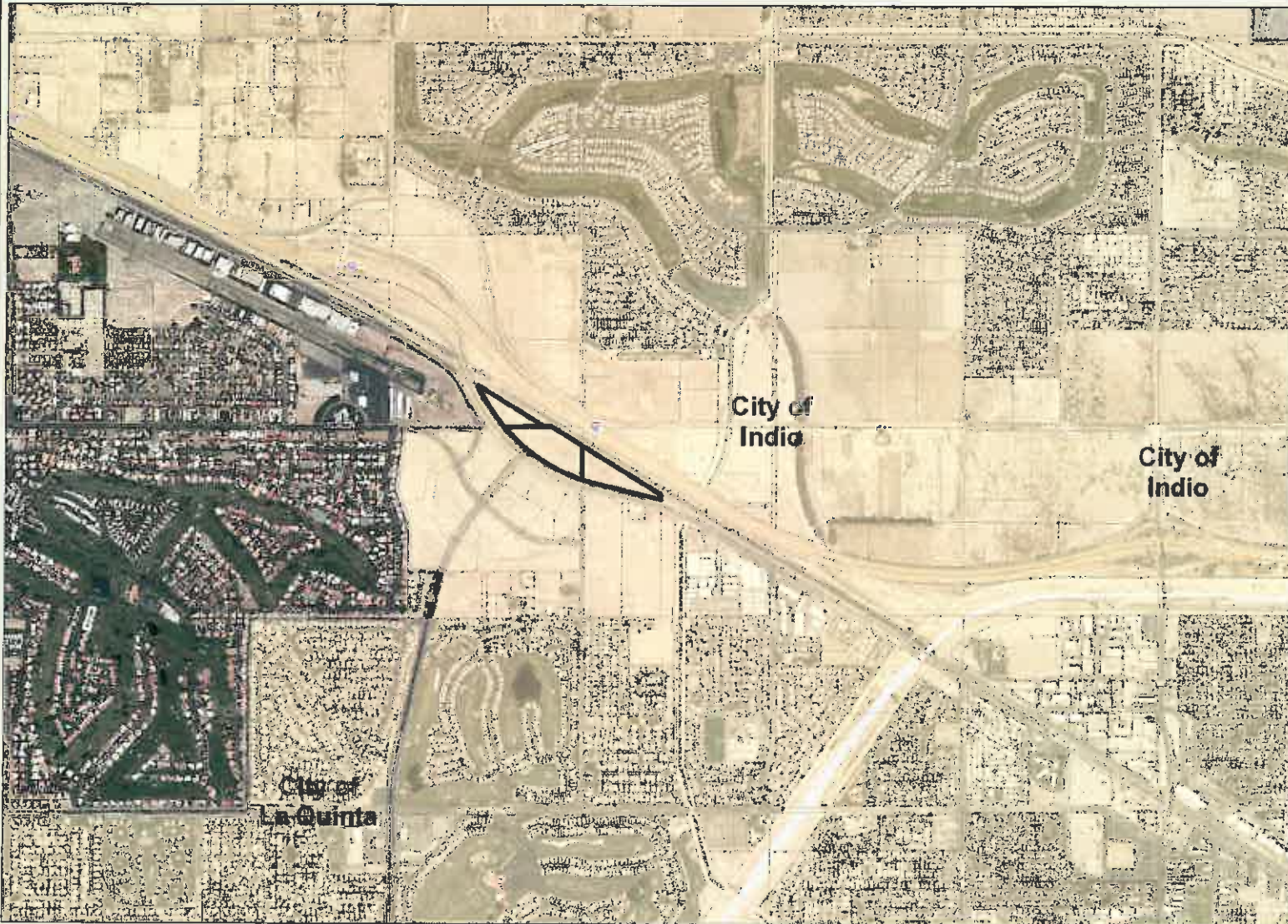
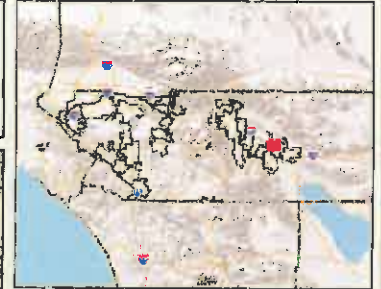
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/10/2016 1:11:22 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- City Boundaries
- Cities**
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads**
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 2,568 5,136 Feet



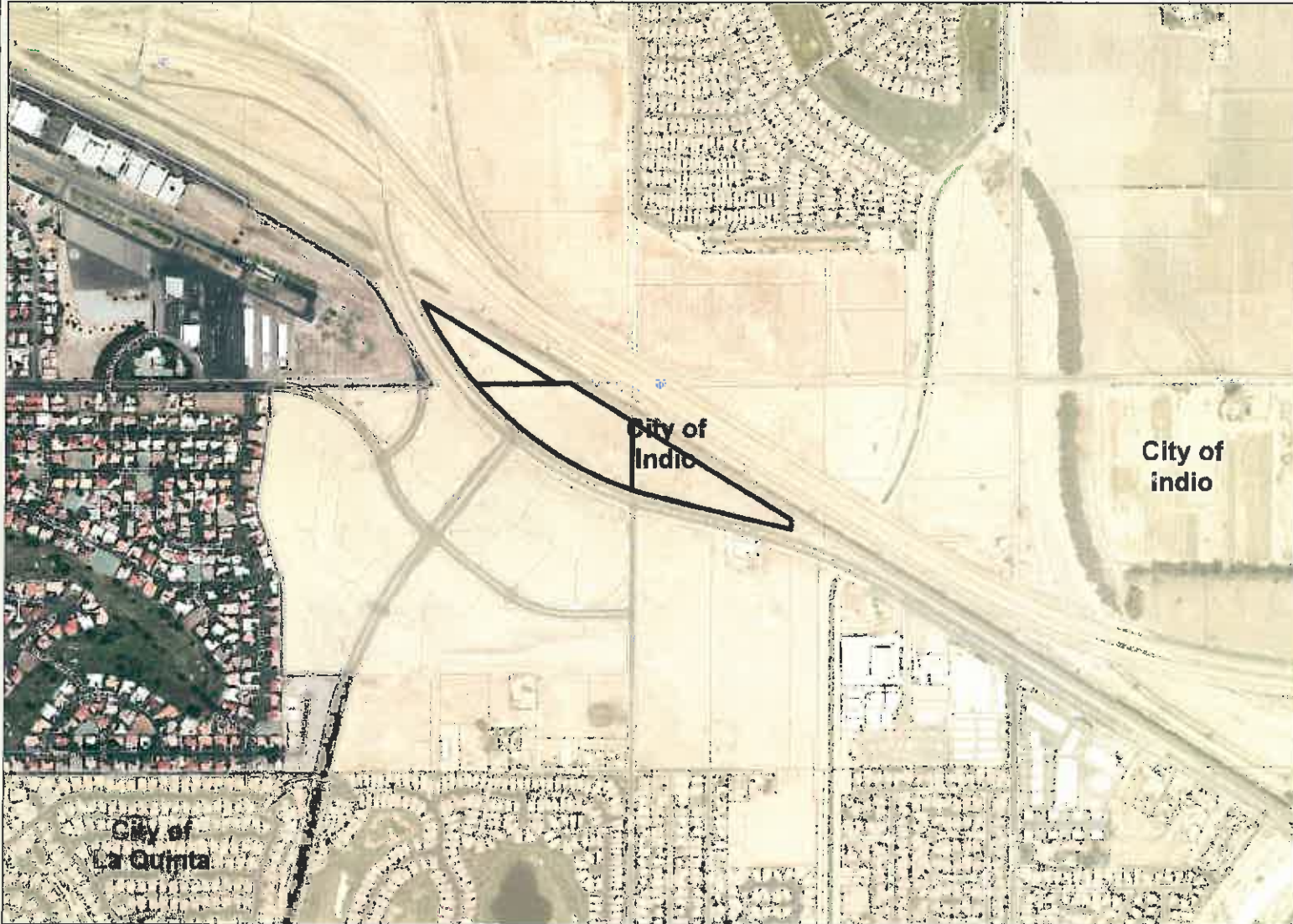
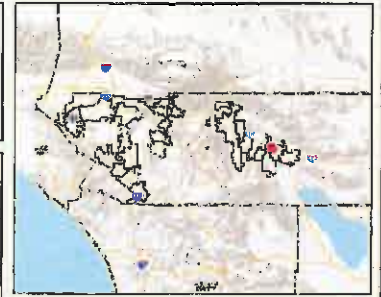
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/10/2016 1:14:07 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 1,284 2,568 Feet



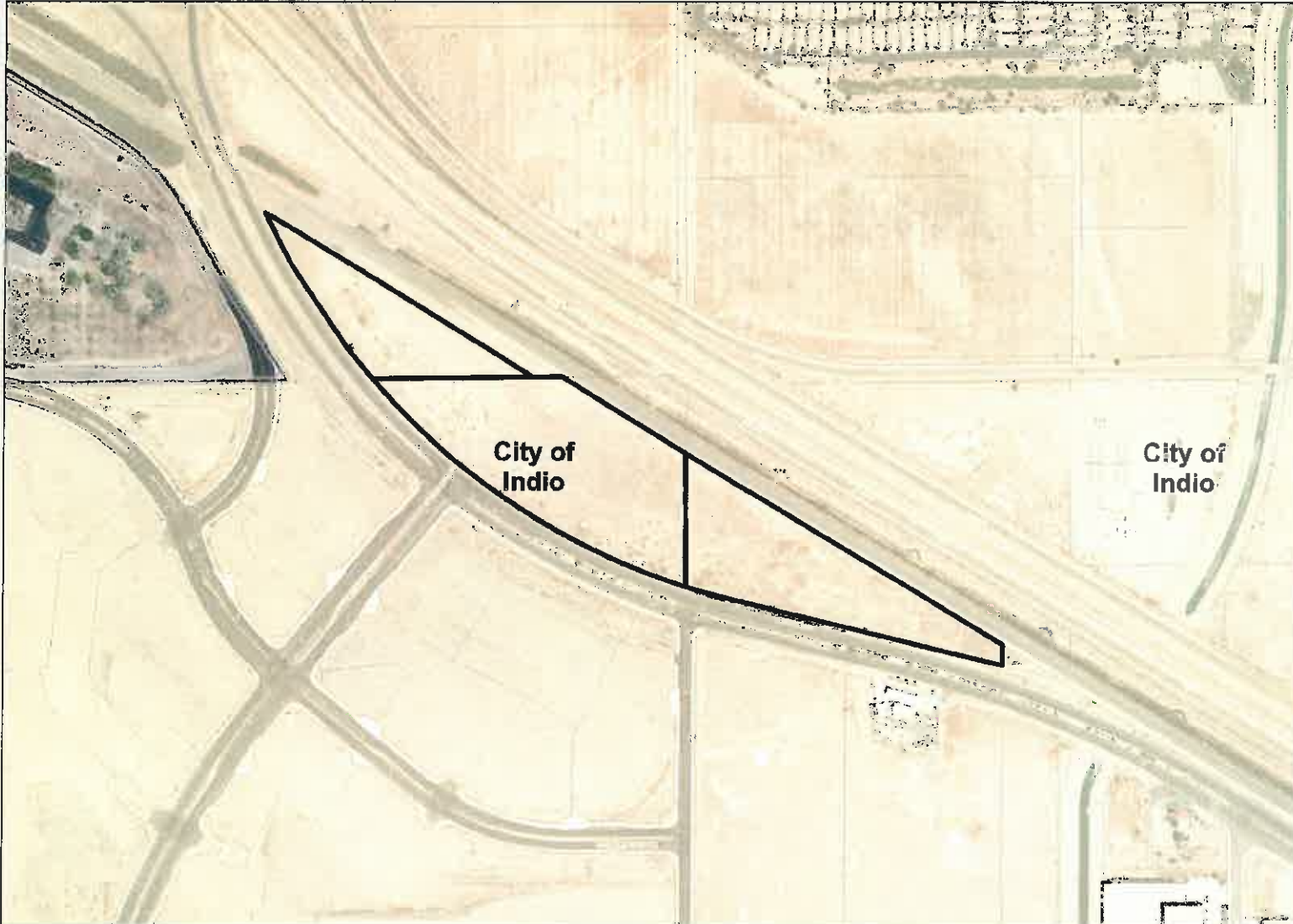
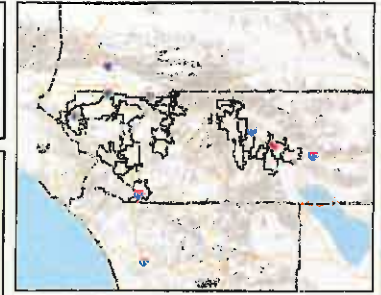
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/10/2016 1:10:11 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities**
- roadsanno**
- highways**
 - HWY
 - INTERCHANGE
 - INTERSTATE
 - OFFRAMP
 - ONRAMP
 - USHWY
- counties
- cities
- hydrographylines**
- waterbodies**
 - Lakes
 - Rivers



0 642 1,284 Feet

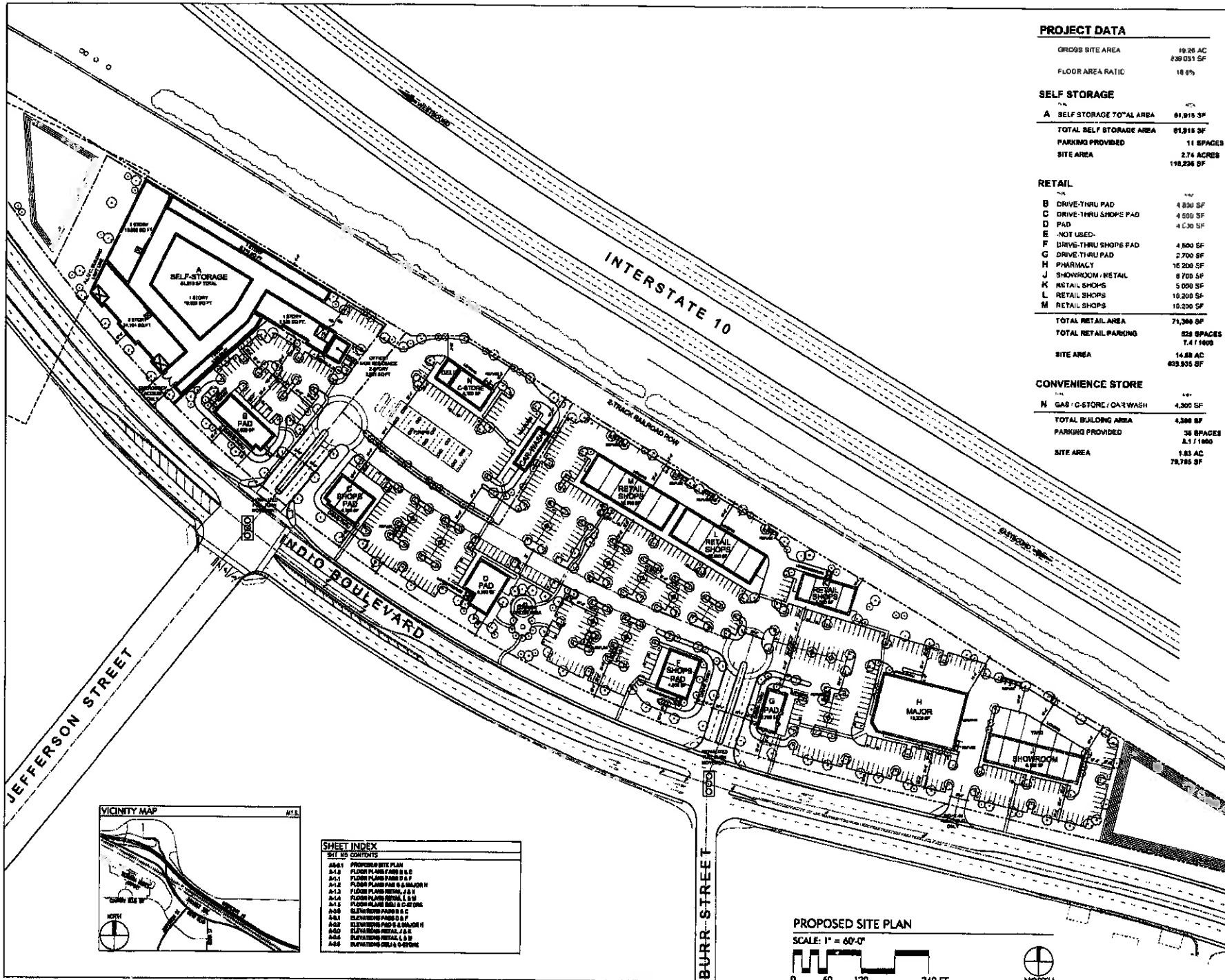


IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

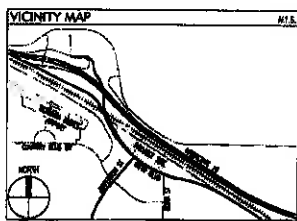
REPORT PRINTED ON... 11/10/2016 1:15:11 PM

© Riverside County RCIT GIS

Notes



PROJECT DATA	
GROSS SITE AREA	19.26 AC
	200,031 SF
FLOOR AREA RATIO	18.8%
SELF STORAGE	
N/A	
A SELF STORAGE TOTAL AREA	81,916 SF
TOTAL SELF STORAGE AREA	81,916 SF
PARKING PROVIDED	11 SPACES
SITE AREA	2.74 ACRES
	118,208 SF
RETAIL	
N/A	
B DRIVE-THRU PAD	4,800 SF
C DRIVE-THRU SHOPE PAD	4,500 SF
D PAD	4,000 SF
E NOT USED	
F DRIVE-THRU SHOPE PAD	4,800 SF
G DRIVE-THRU PAD	2,700 SF
H PHARMACY	16,200 SF
J SHOWROOM - RETAIL	9,700 SF
K RETAIL SHOPS	5,000 SF
L RETAIL SHOPS	10,200 SF
M RETAIL SHOPS	10,200 SF
TOTAL RETAIL AREA	71,300 SF
TOTAL RETAIL PARKING	828 SPACES
	7.41 ACRES
SITE AREA	14.88 AC
	639,935 SF
CONVENIENCE STORE	
N/A	
N GAS / C-STORE / CARWASH	4,300 SF
TOTAL BUILDING AREA	4,300 SF
PARKING PROVIDED	36 SPACES
	3.17 ACRES
SITE AREA	1.83 AC
	79,785 SF

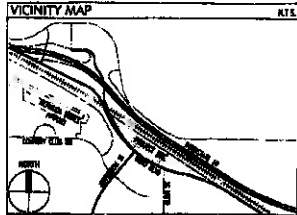
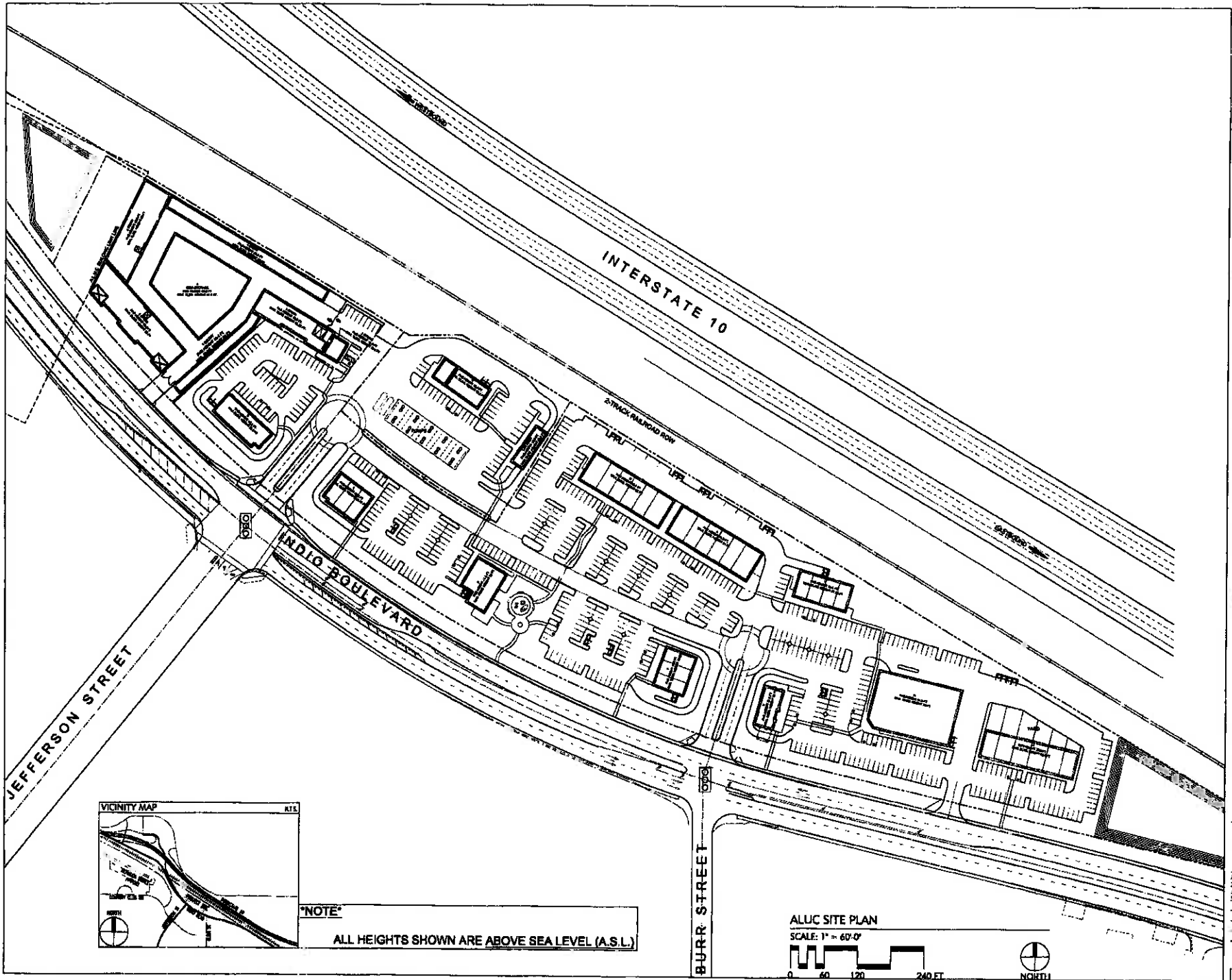


SHEET INDEX	
SHEET NO. CONTENTS	
A-0.0	PROPOSED SITE PLAN
A-0.1	FLOOR PLANS PADS B & C
A-0.2	FLOOR PLANS PADS D & F
A-0.3	FLOOR PLANS PADS G & MAJOR H
A-0.4	FLOOR PLANS RETAIL J, K & L
A-0.5	FLOOR PLANS RETAIL M & CONVENIENCE
A-0.6	ELEVATIONS PADS B & C
A-0.7	ELEVATIONS PADS D & F
A-0.8	ELEVATIONS PADS G & MAJOR H
A-0.9	ELEVATIONS RETAIL J, K & L
A-1.0	ELEVATIONS RETAIL M & CONVENIENCE

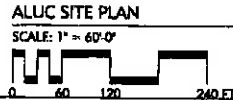
PROPOSED SITE PLAN
 SCALE: 1" = 60'-0"
 0 60 120 240 FT. NORTH

JEFFERSON STREET PROJECT
 Commercial Development
 Indio Boulevard & Jefferson Street
 Indio, California

DATE: 10/15/10
 SCALE: AS SHOWN
 SHEET NO. A-0.1
 PROJECT NO. 10-001
 A-0.1
 PROPOSED SITE PLAN



NOTE
 ALL HEIGHTS SHOWN ARE ABOVE SEA LEVEL (A.S.L.)



The Geo. Stanton Architects
 1530 West Lewis St.
 West Platte, CO 81050
 Tel: 619.239.9111
 www.stanton.com

JEFFERSON STREET PROJECT
 Commercial Development
 Indio Boulevard & Jefferson Street
 Indio, California

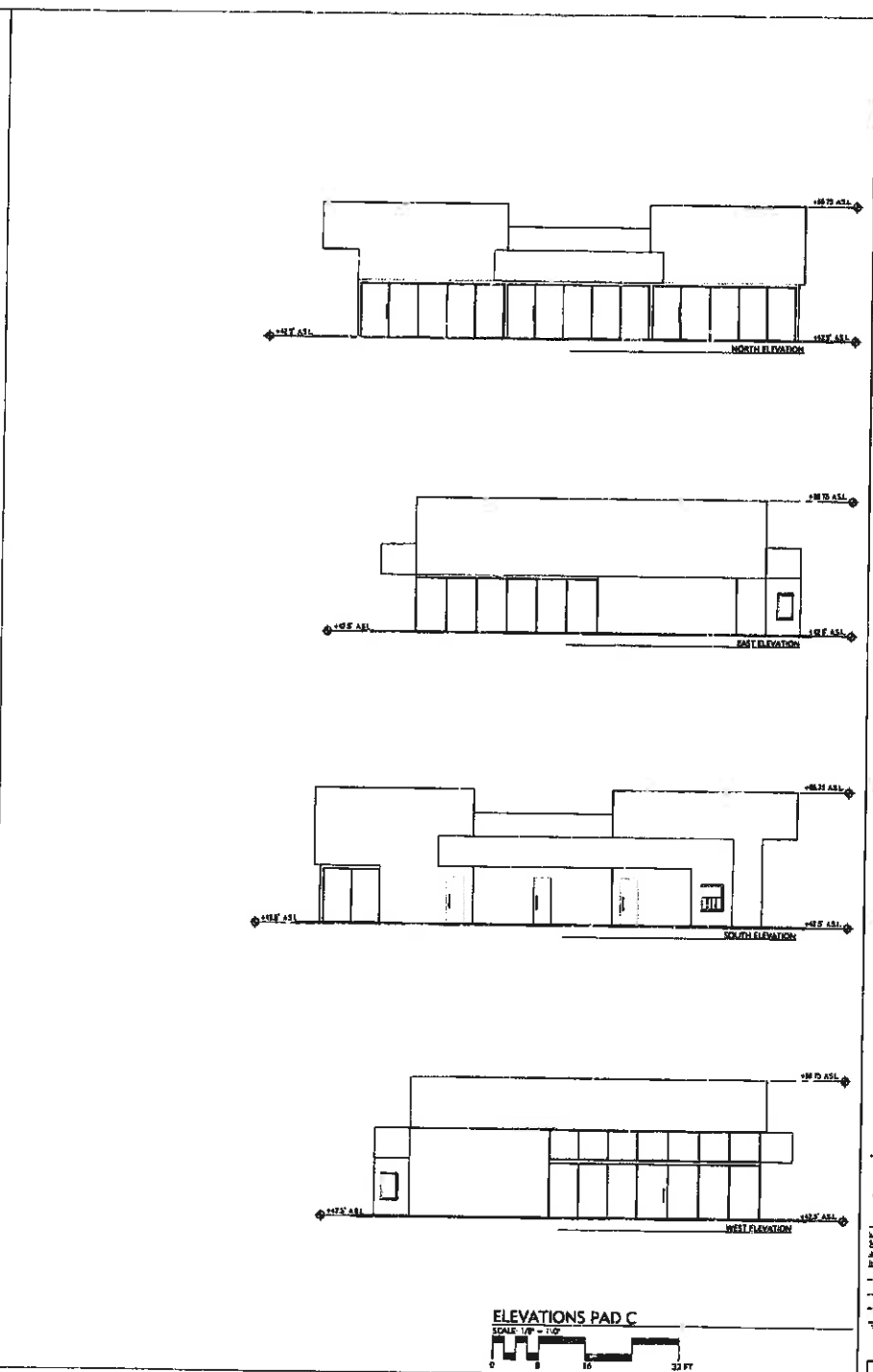
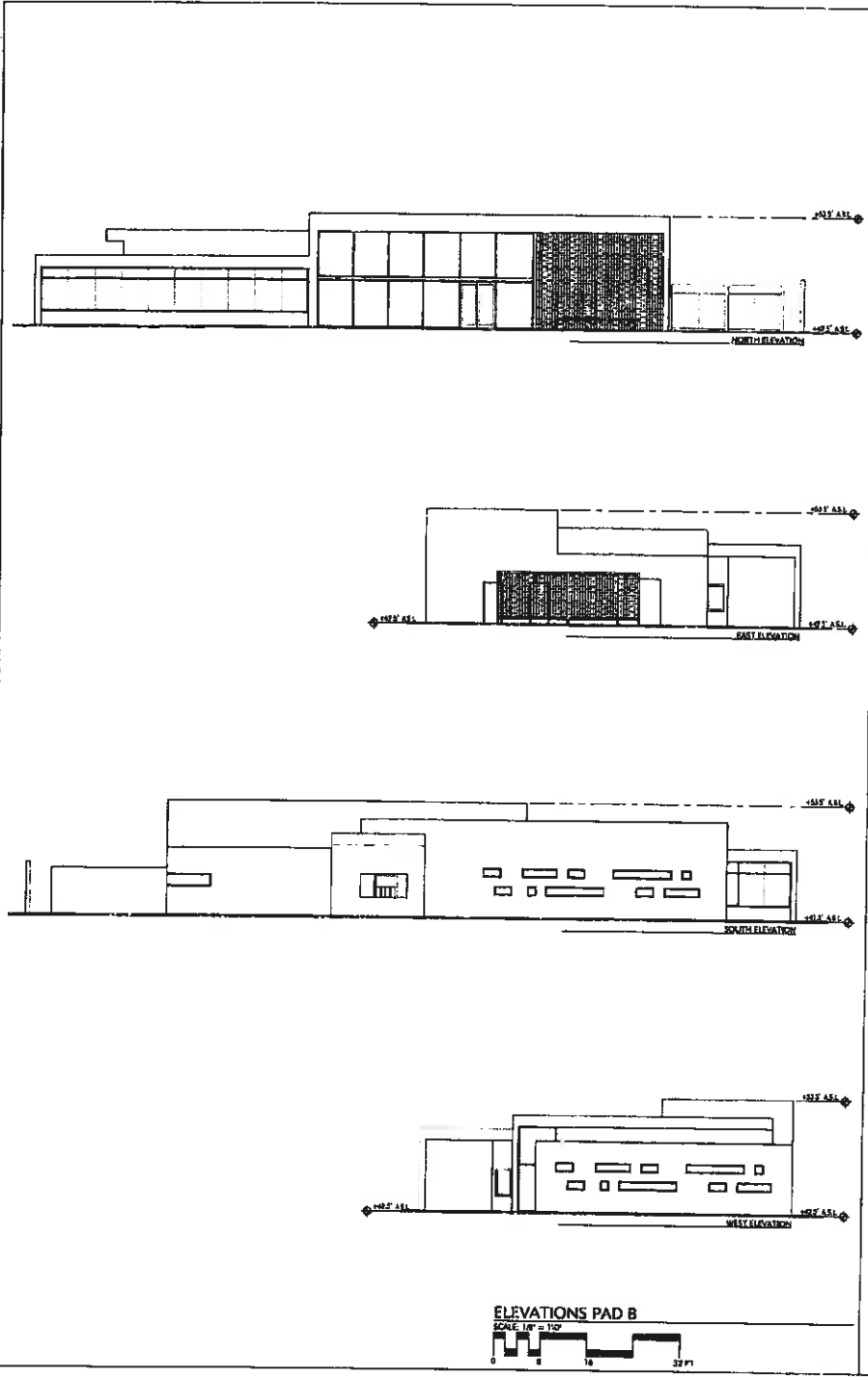
Project No.	15-001
Client	Indio City
Scale	1" = 60'-0"
Date	10/20/15
Sheet No.	A-0.1
Sheet Title	ALLIC SITE PLAN

McQue Station Architects
 1530 West Park Rd.
 San Bruno, CA 94068
 Tel: 619.299.8111
 www.mcquestation.com

JEFFERSON STREET PROJECT
 Commercial Development
 Indio Boulevard & Jefferson Street
 Indio, California

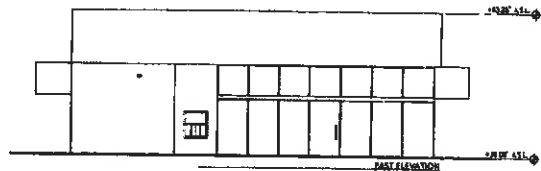
DATE: 11/11/11
 DRAWING NO: A-3.0
 SHEET NO: 1
 TOTAL SHEETS: 1
 PROJECT NO: 11-0001
 CLIENT: [REDACTED]
 ARCHITECT: McQue Station Architects
 ENGINEER: [REDACTED]
 CONTRACTOR: [REDACTED]

A-3.0
 SHEET NO: 1
 ELEVATIONS PAD C

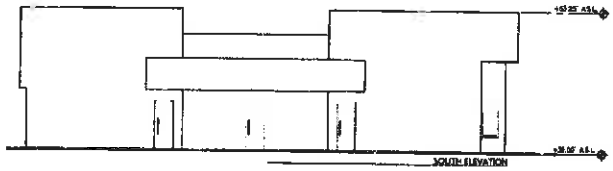




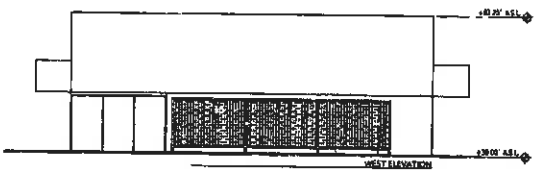
NORTH ELEVATION



EAST ELEVATION



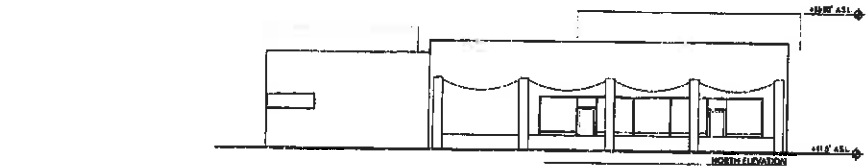
SOUTH ELEVATION



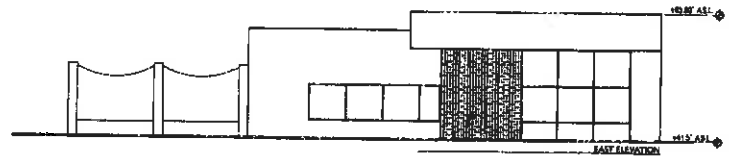
WEST ELEVATION

ELEVATIONS PAD F

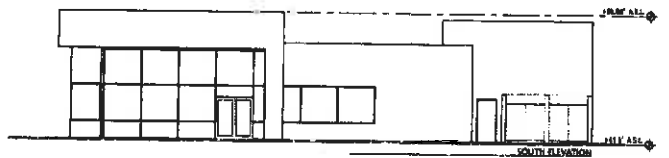
SCALE: 1/8" = 1'-0"



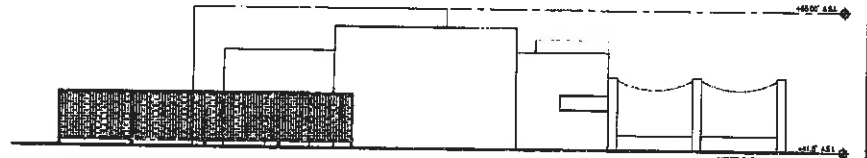
NORTH ELEVATION



EAST ELEVATION



SOUTH ELEVATION



WEST ELEVATION

ELEVATIONS PAD D

SCALE: 1/8" = 1'-0"



McGee + Kraton Architects
 1330 West Lake St.
 San Diego, CA 92103
 Tel: 619.299.9111
 mcg@mcgk.com

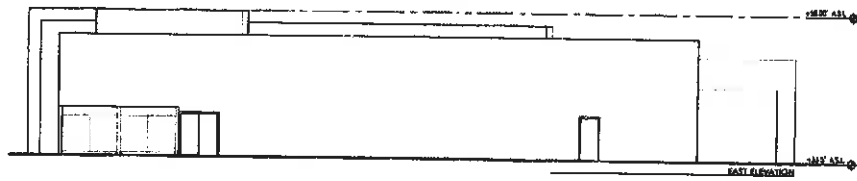
JEFFERSON STREET PROJECT

Commercial Development

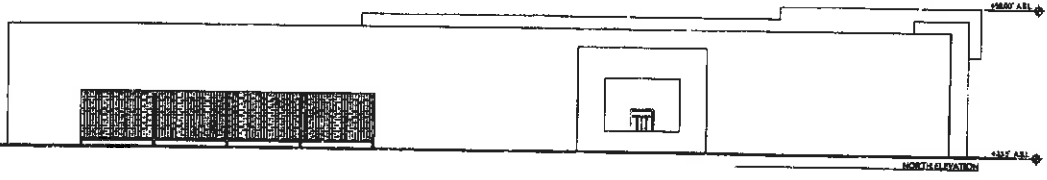
Indio Boulevard & Jefferson Street
 Indio, California

Prepared by: [Name]
 Checked by: [Name]
 Date: [Date]
 Scale: [Scale]
 Title: [Title]

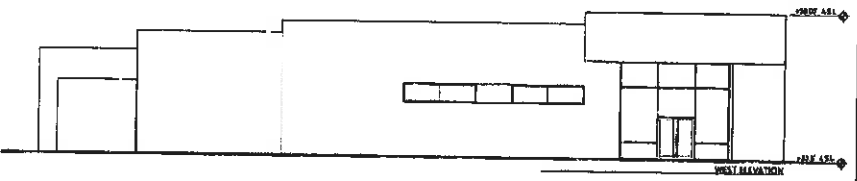
A-3.1
 SHEET 1 OF 1
 ELEVATIONS PAD



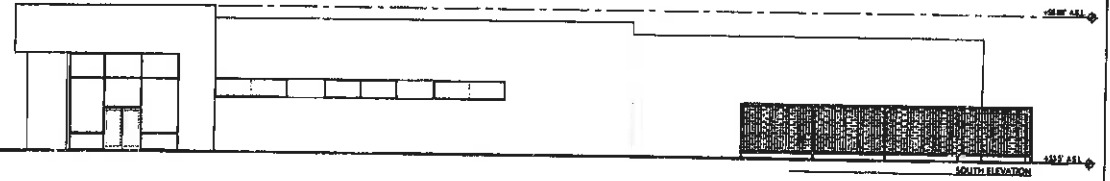
EAST ELEVATION



NORTH ELEVATION

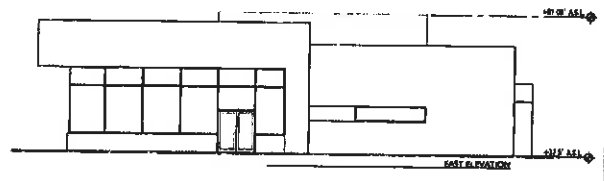


WEST ELEVATION

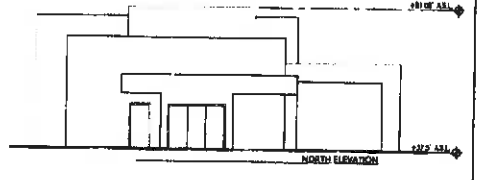


SOUTH ELEVATION

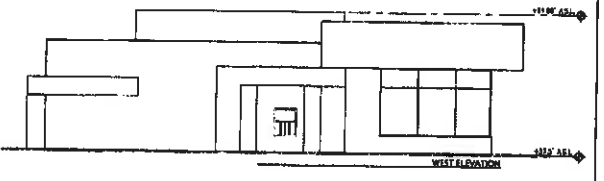
ELEVATIONS MAJOR H



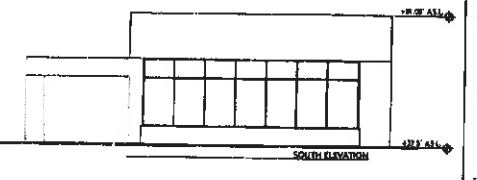
EAST ELEVATION



NORTH ELEVATION



WEST ELEVATION



SOUTH ELEVATION

ELEVATIONS PAD G

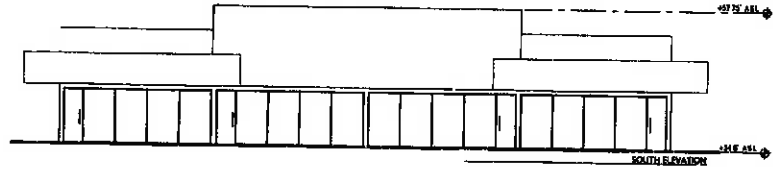
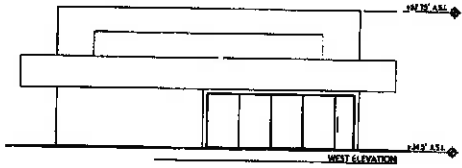
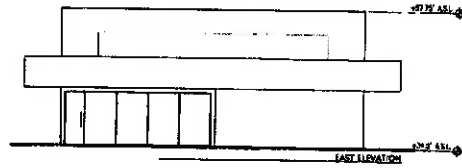


McQue - Stanton Architects
 1500 West 104th St.
 San Mateo, CA 94025
 Tel: 650.239.5111
 mquestanton.com

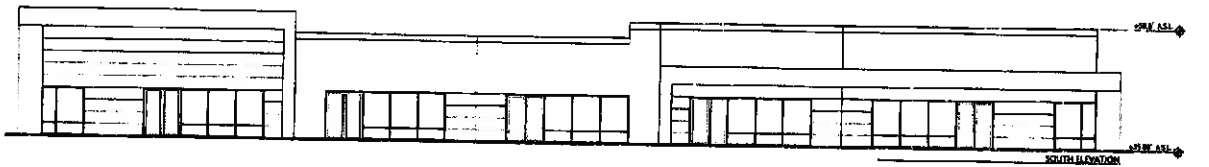
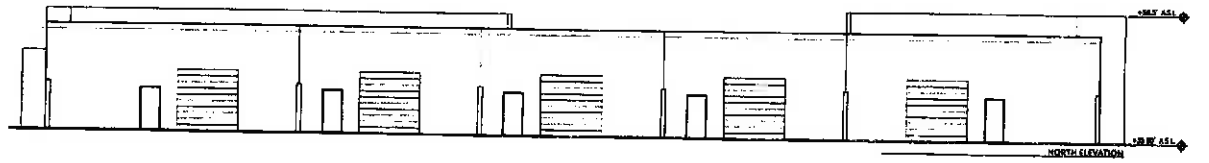
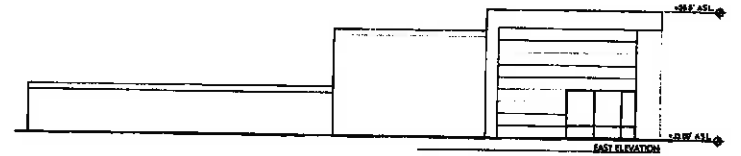
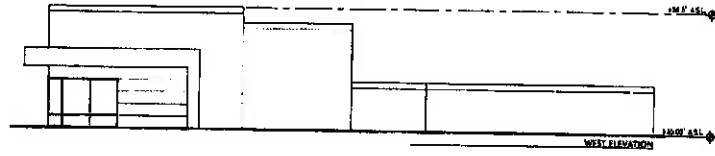
JEFFERSON STREET PROJECT
 Commercial Development
 Indio Boulevard & Jefferson Street
 Indio, California

Prepared by:	Checked by:
Designed by:	Reviewed by:
Drafted by:	Approved by:
Date:	Scale:
Sheet No.:	Project No.:
Project Name: JEFFERSON STREET PROJECT Location: Indio Boulevard & Jefferson Street, Indio, California	

A-3.2
 SHEET 01
 ELEVATIONS



ELEVATIONS RETAIL K



ELEVATIONS RETAIL J



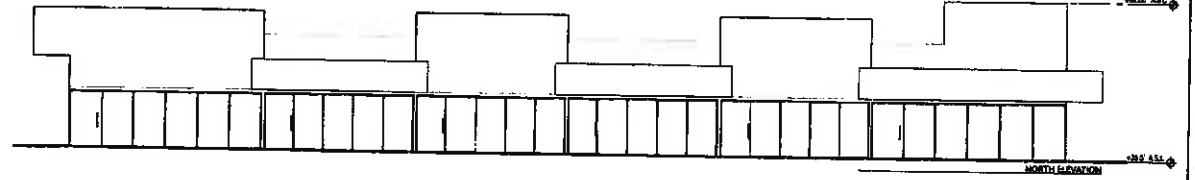
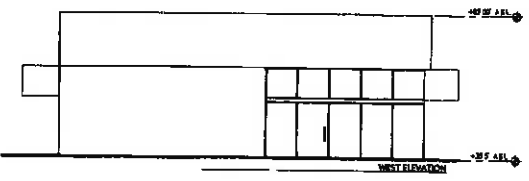
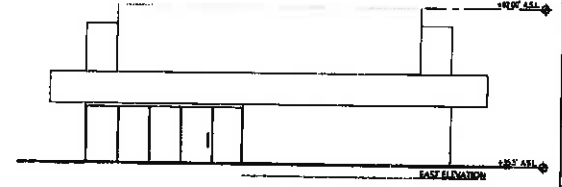
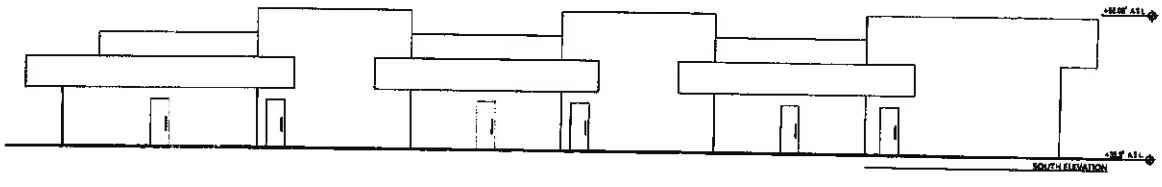
The Geo. Shattuck Architects
 1500 West 14th St.
 San Mateo, CA 94402
 Tel: 650.259.9111
 info@shattuck.com

JEFFERSON STREET PROJECT
 Commercial Development
 Indio Boulevard & Jefferson Street
 Indio, California

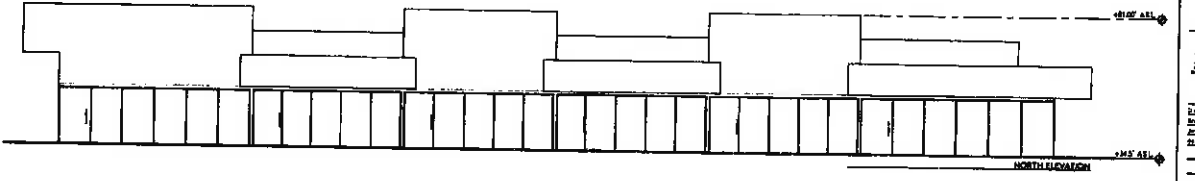
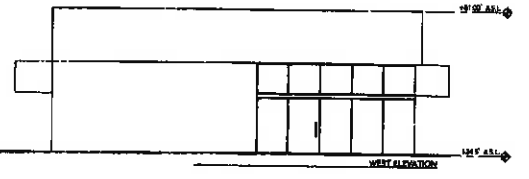
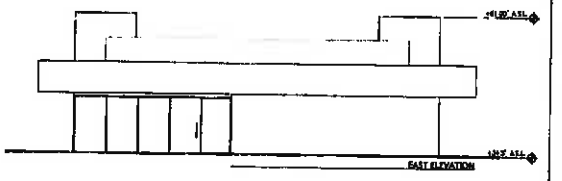
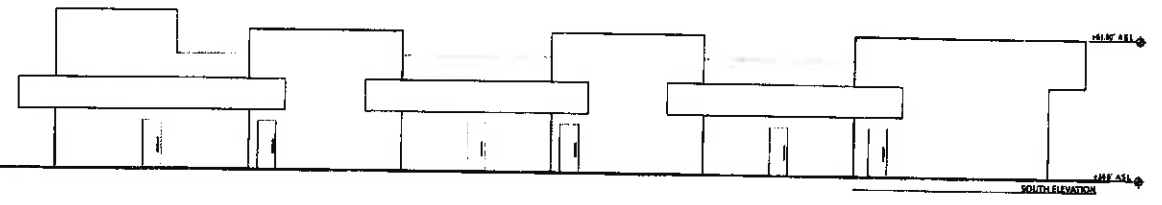
SHEET NO. A-3.3
 WEST 1/8" = 1'-0"
 ELEVATIONS
 RETAIL J, K

The Geo-Technion Architect
 1330 West Tower St.
 San Diego, CA 92102
 Tel: 619.289.5111
 www.geotechnion.com

JEFFERSON STREET PROJECT
 Commercial Development
 Indio Boulevard & Jefferson Street
 Indio, California

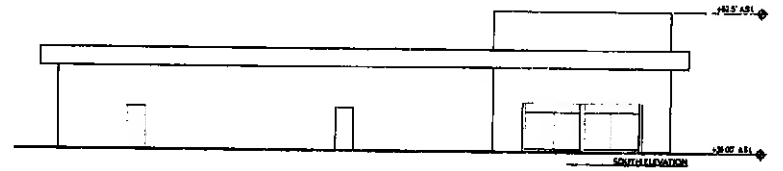
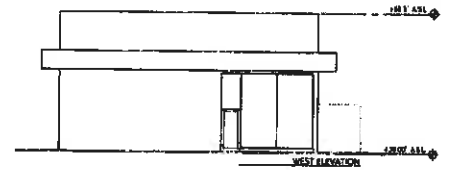
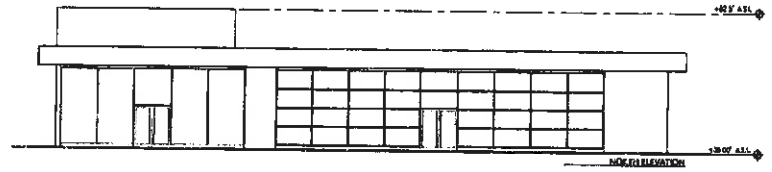
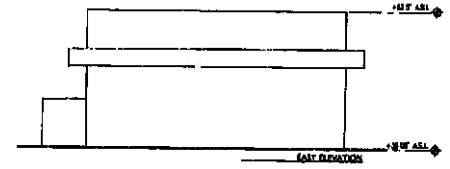
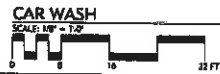
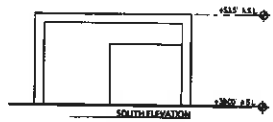
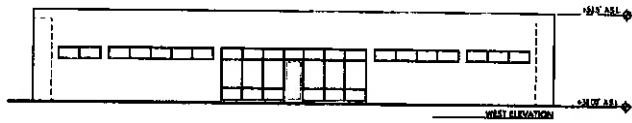
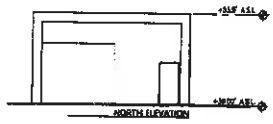


ELEVATIONS RETAIL M
 SCALE: 1/8" = 1'-0"
 0 8 16 24 FT



ELEVATIONS RETAIL L
 SCALE: 1/8" = 1'-0"
 0 8 16 24 FT

DATE: 11/11/11
 DRAWN BY: [Name]
 CHECKED BY: [Name]
 PROJECT NO.: [Number]
 SHEET NO.: [Number]
 TOTAL SHEETS: [Number]
A-3.4
 WEST 1 OF 1
 DRAWINGS
 11/11/11

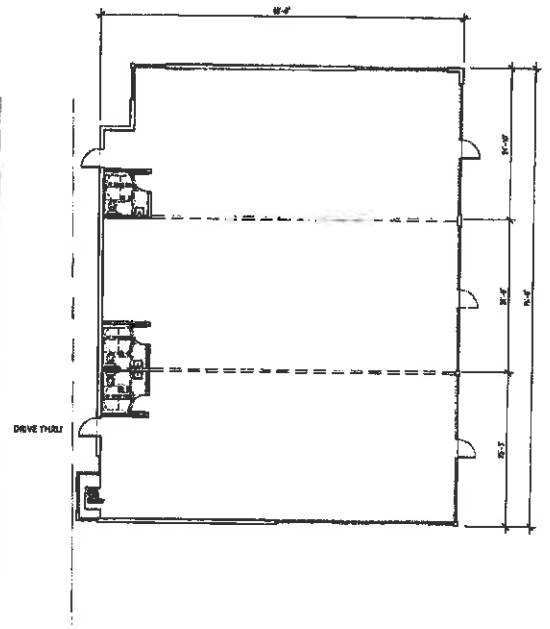


McGee Stanton Architects
1830 West 14th St.
San Diego, CA 92103
Tel: 619-239-3111
mgs@stanton.com

JEFFERSON STREET PROJECT
Commercial Development
Indio Boulevard & Jefferson Street
Indio, California

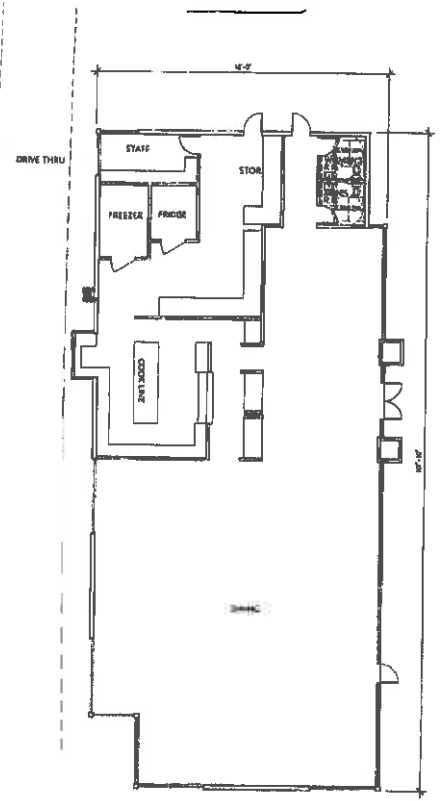
DATE: 11/11/11
DRAWN BY: J. J. JONES
CHECKED BY: J. J. JONES
SCALE: 1/8" = 1'-0"

A-3.5
ELEVATIONS
DELI & C-STORE



FLOOR PLAN PAD C

SCALE: 1/8" = 1'-0"



FLOOR PLAN PAD B

SCALE: 1/8" = 1'-0"

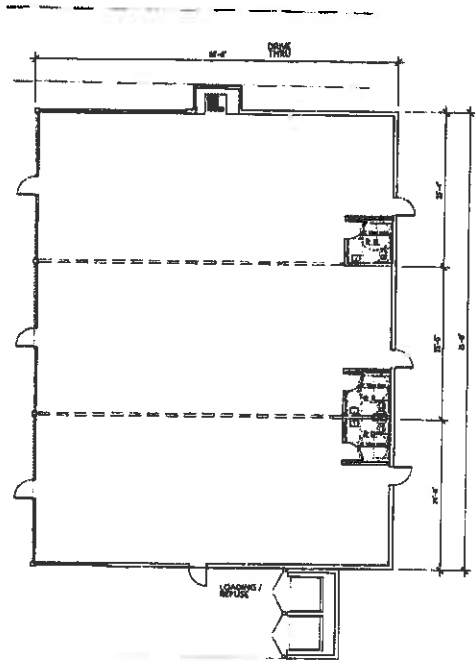


JEFFERSON STREET PROJECT
 Commercial Development
 Indio Boulevard & Jefferson Street
 Indio, California

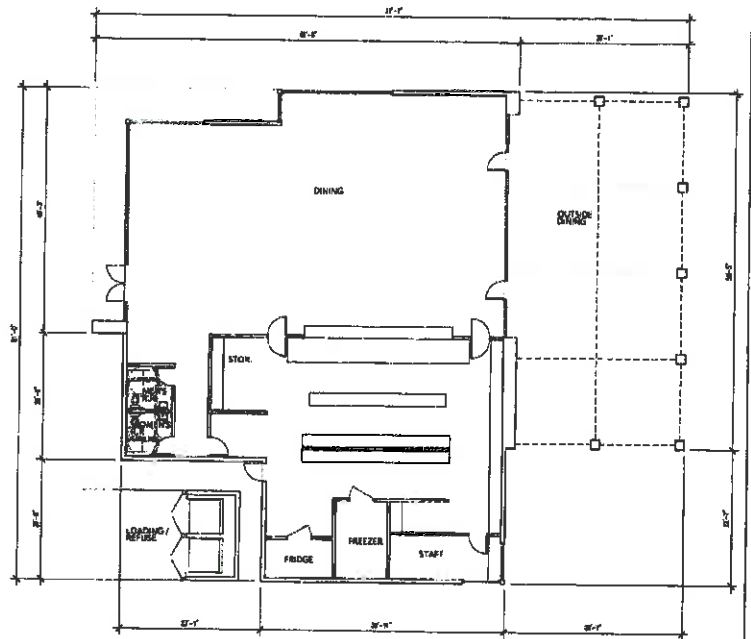
Project No.	1000000000
Client	Indio City
Scale	As Shown
Rev.	001
Date	08/10/11
Drawn	ML
Checked	ML
Approved	ML

Sheet No. **A-1.0**
 SHEET 1 OF 1
 FLOOR PLANS B & C

McGee Skidmore Architects
 1530 West Loma St.
 San Diego, CA 92103
 Tel: 619.299.9111
 mcgeesk.com



FLOOR PLAN PAD F



FLOOR PLAN PAD D



JEFFERSON STREET PROJECT
 Commercial Development
 Indio Boulevard & Jefferson Street
 Indio, California

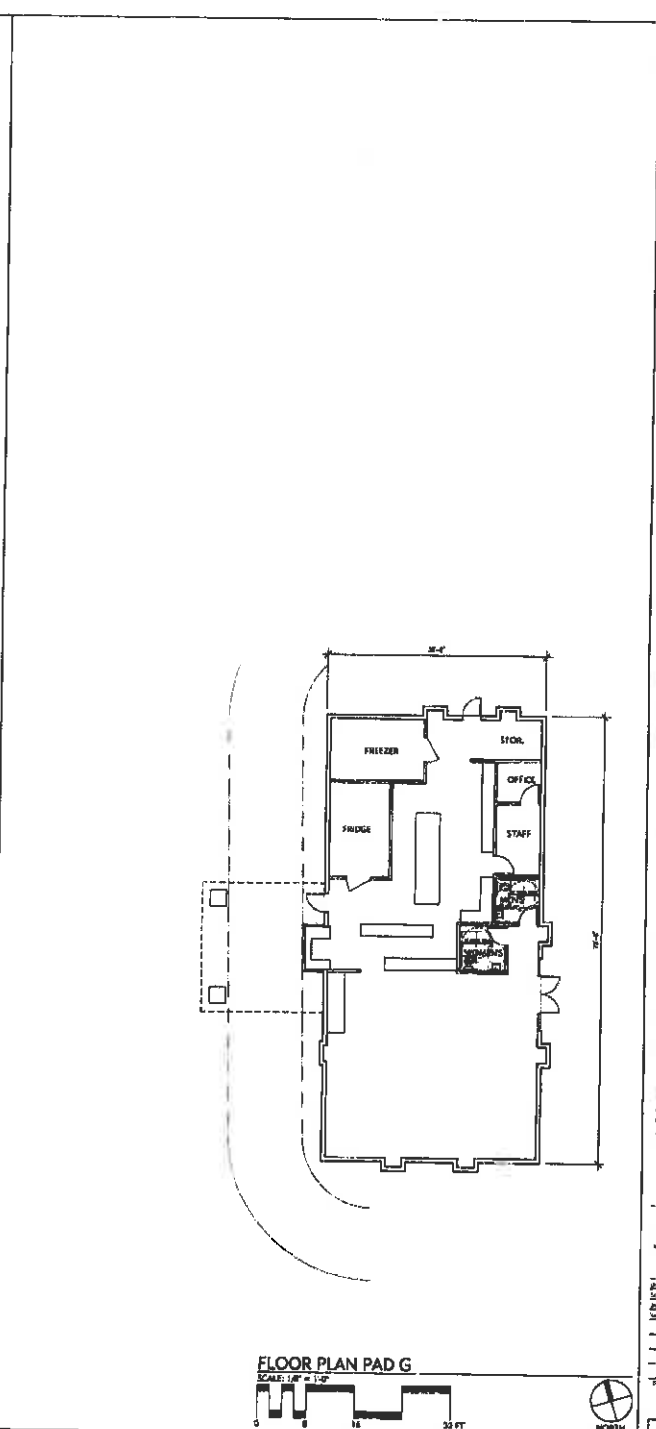
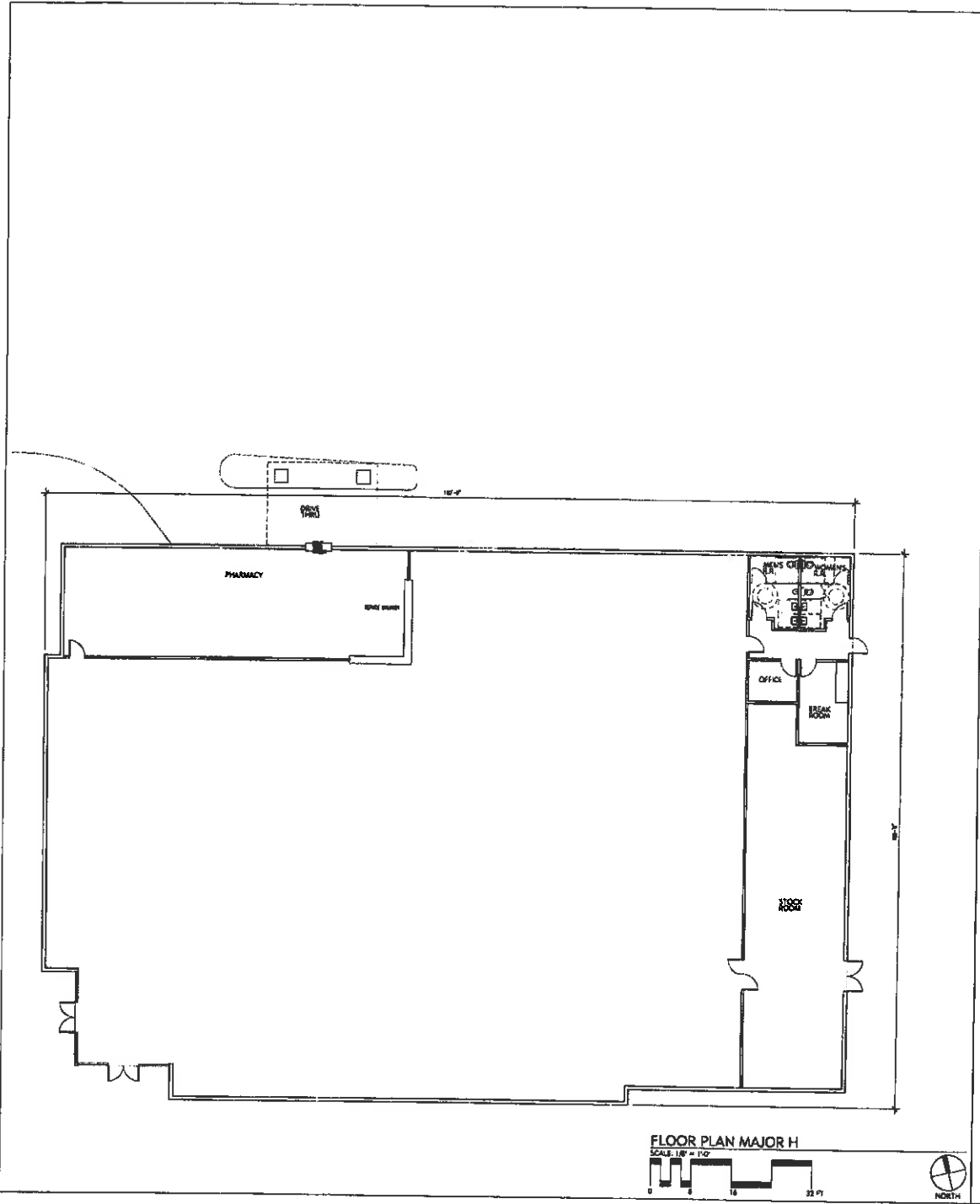
DATE: 11/11/11
 DRAWN BY: J. J. J.
 CHECKED BY: J. J. J.
 SCALE: 1/4" = 1'-0"

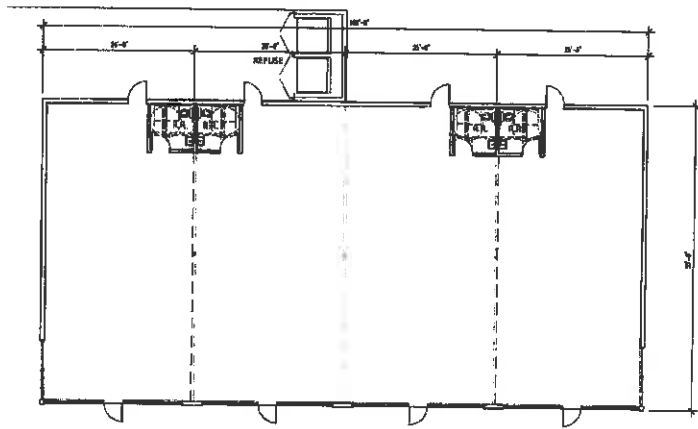
Sheet number: **A-1.1**
 SHEET 1 OF 1
 FLOOR PLANS D & F

McGehee, Dutton, & Hochstetler
 1330 West Lewis St.
 San Diego, CA 92103
 TEL: 619.299.9111
 mcgeehedutton.com

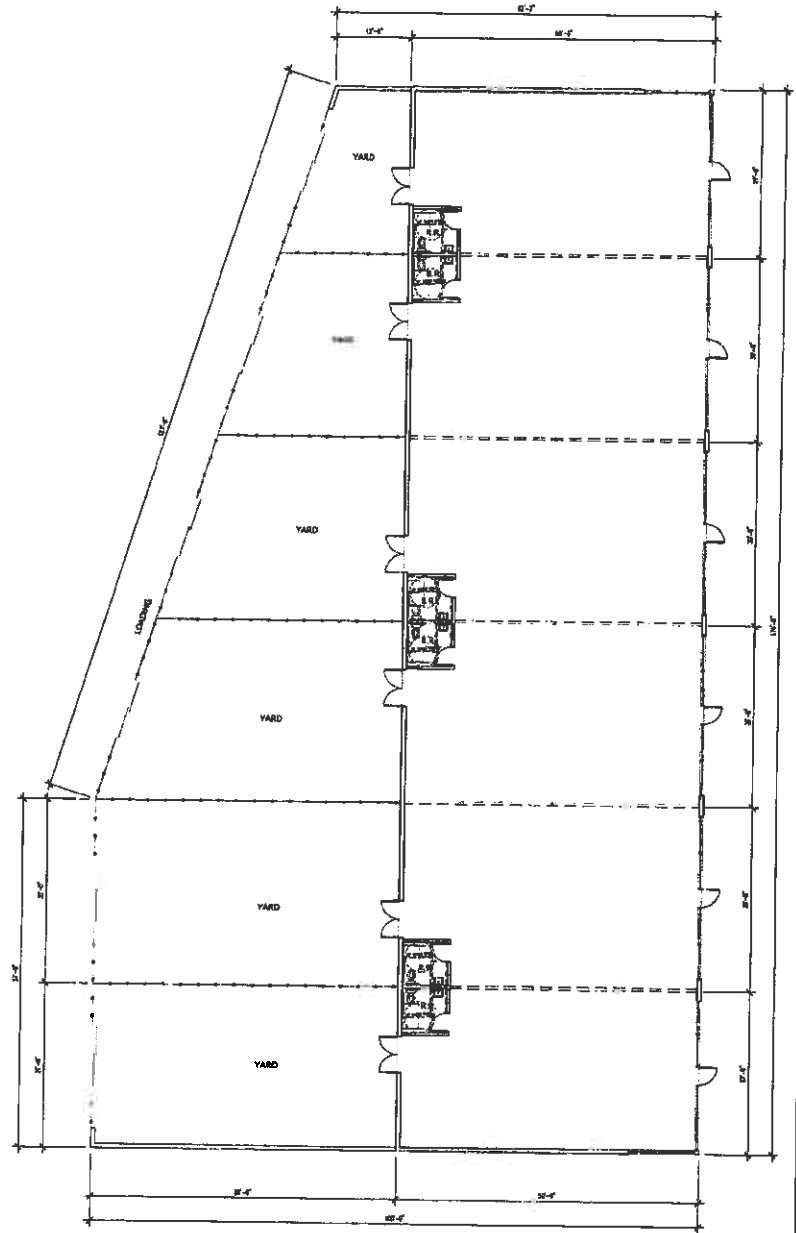
JEFFERSON STREET PROJECT
 Commercial Development
 Indio Boulevard & Jefferson Street
 Indio, California

Project No.	
Client	
Architect	McGee Stanton Architects
Date	
Scale	1/8" = 1'-0"
Sheet	A-1.2
Project Name	Jefferson Street Project
Location	Indio, California
Author	
Checked	
Approved	





FLOOR PLAN RETAIL K



FLOOR PLAN RETAIL J



JEFFERSON STREET PROJECT

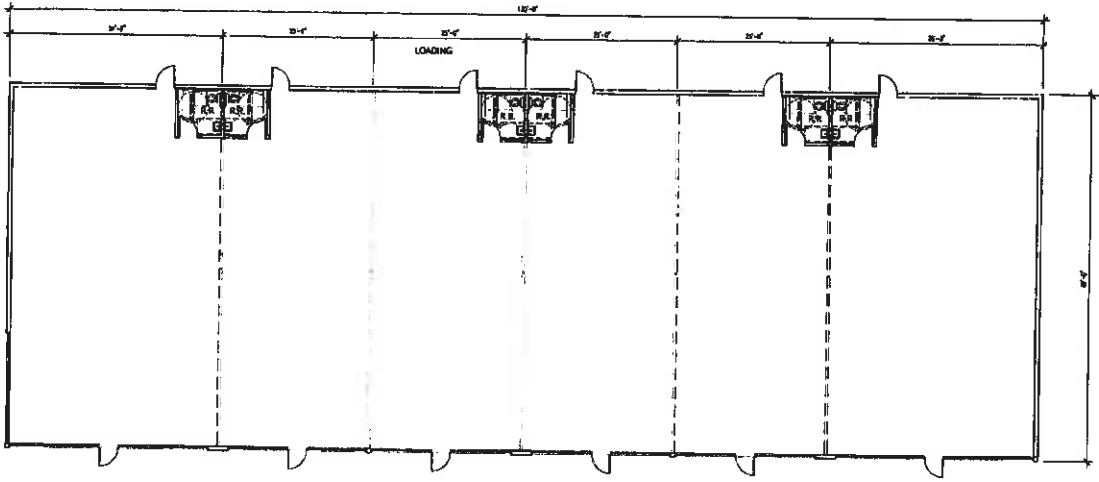
Commercial Development

Indio Boulevard & Jefferson Street
Indio, California

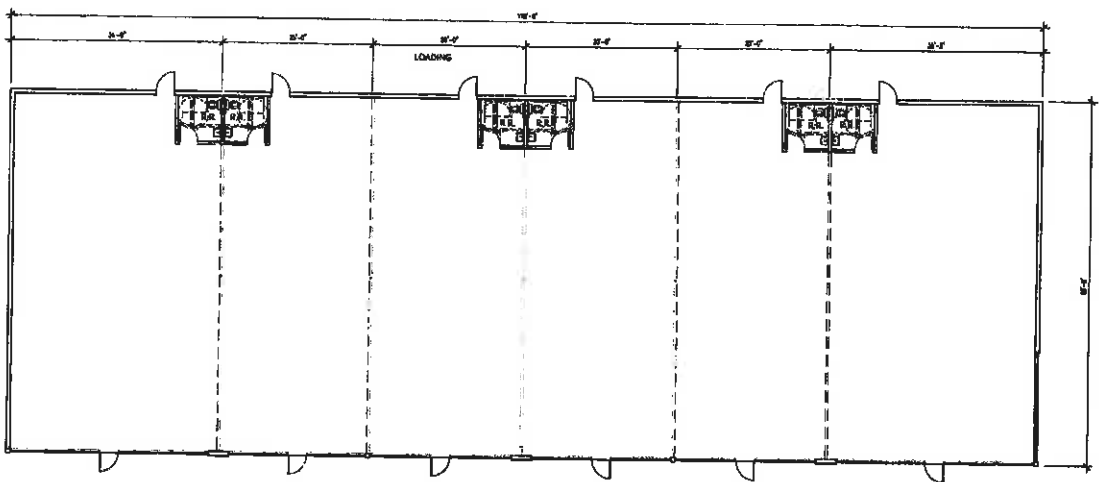
Prepared by	Checked by
Drawn by	Reviewed by
Date	Scale
Sheet	Project

A-13
SUBJECT:
FLOOR PLANS E & K

McCoy Stanton Architects
1530 West Lewis St.
San Diego, CA 92103
Tel: 619.299.3111
mcsa@stan.com



FLOOR PLAN RETAIL M



FLOOR PLAN RETAIL L



McGee Skidmore Architects
 1370 West 24th St.
 San Mateo, CA 94403
 Tel: 650-279-9111
 mcgeeskidmore.com

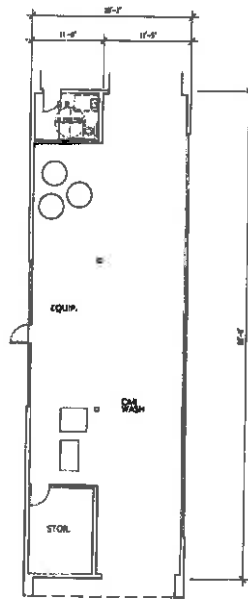
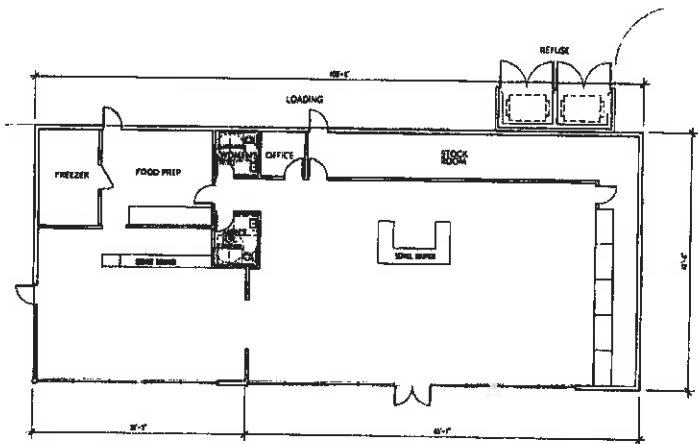
JEFFERSON STREET PROJECT
 Commercial Development

Indio Boulevard & Jefferson Street
 Indio, California

Project No.	1370
Client	McGee Skidmore Architects
Architect	McGee Skidmore Architects
Scale	1/8" = 1'-0"
Date	10/10/11
Sheet	A-1.4

A-1.4

FLOOR PLANS L & M



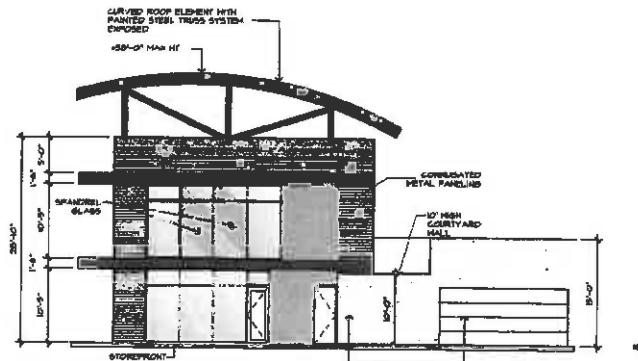
FLOOR PLAN DELI, C-STORE & CAR WASH



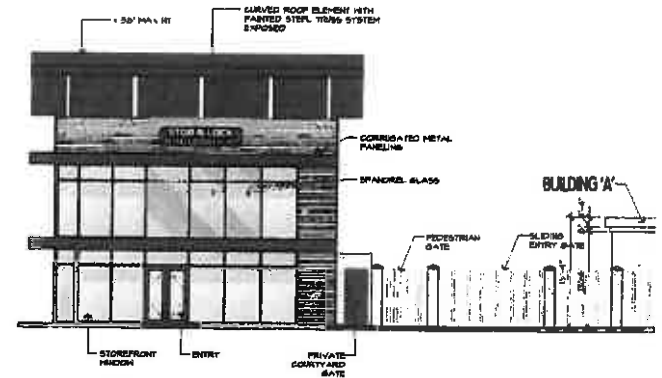
Mc Gee Sharon Architects
 1530 West Parla St.
 San Diego, CA 92103
 Tel: 619.299.5111
 mcgearchitect.com

JEFFERSON STREET PROJECT
 Commercial Development
 Indio Boulevard & Jefferson Street
 Indio, California

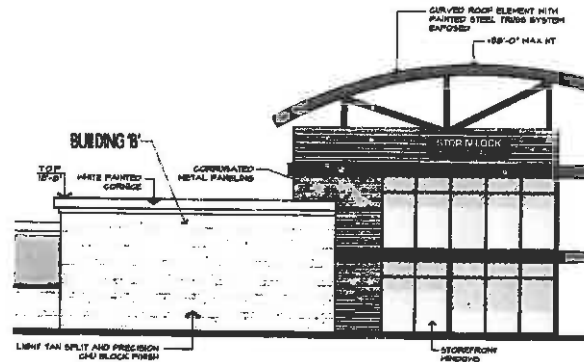
SHEET NO. A-1.5
 TITLE: FLOOR PLANS
 DATE: 08/11/11



OFFICE & RESIDENCE NORTH ELEVATION



OFFICE & RESIDENCE EAST ELEVATION



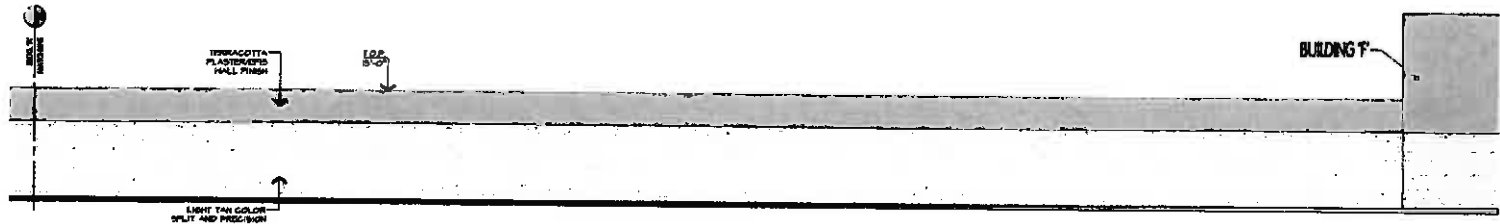
OFFICE & RESIDENCE SOUTH ELEVATION

ALAN JONES
STOR-N-LOCK SELF STORAGE
 INDIO, CA

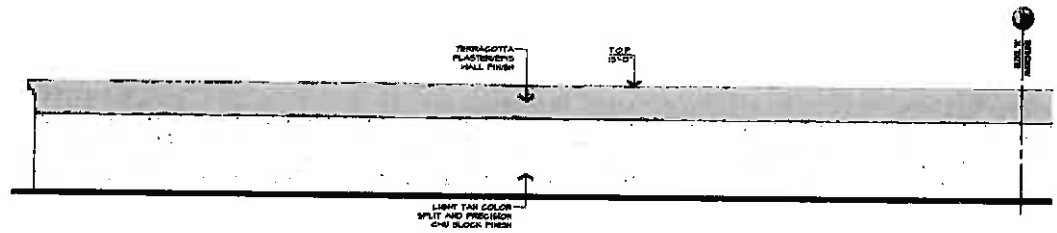
SCHEME A
PRELIMINARY OFFICE & RESIDENCE ELEVATION

NO PART OF THIS DOCUMENT IS TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF JORDAN ARCHITECTS, INC.
 JOB NUMBER: 14-001
 SCALE: 1/8"=1'-0"
 DATE: 10/24/2014

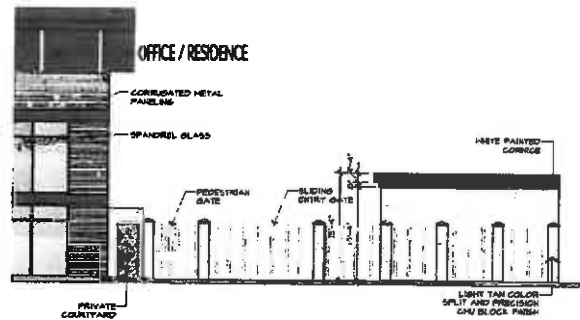




BLDG. 'A' NORTHWEST ELEVATION



BLDG. 'A' NORTHWEST ELEVATION



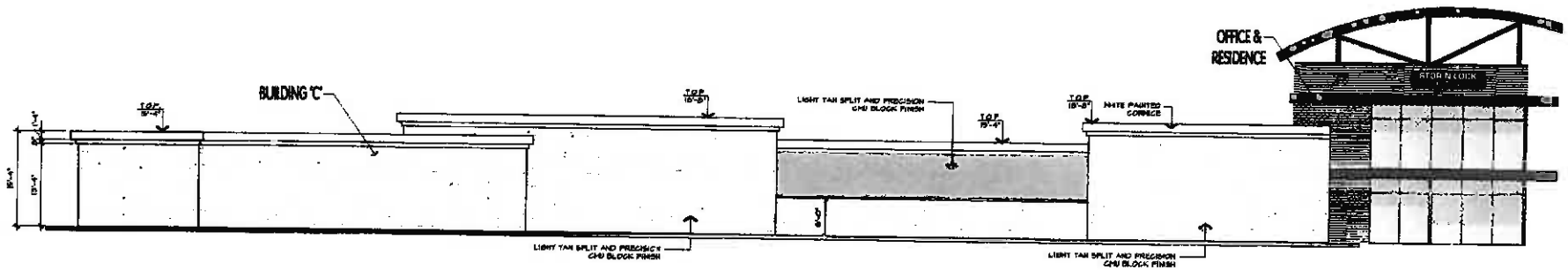
BLDG. 'A' SOUTHEAST ELEVATION

ALAN JONES
STOR-N-LOCK SELF STORAGE
 INDIO, CA

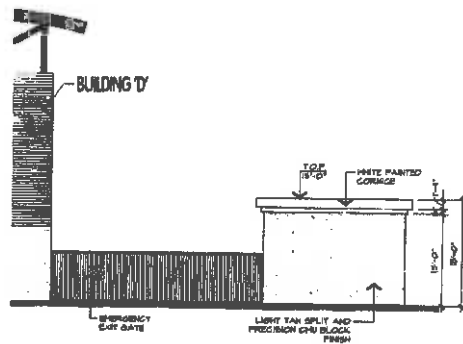
SCHEME A
PRELIMINARY BLDG. 'A' ELEVATIONS

THIS DRAWING IS A PRELIMINARY DESIGN AND SHOULD NOT BE USED FOR CONSTRUCTION. ANY CHANGES TO THIS DESIGN SHALL BE MADE BY THE ARCHITECT. ANY REVISIONS TO THIS DRAWING SHALL BE MADE BY THE ARCHITECT. ANY REVISIONS TO THIS DRAWING SHALL BE MADE BY THE ARCHITECT.
 JOB NUMBER: 14-021
 SCALE: 1/4"=1'-0"
 DATE: 10/24/14





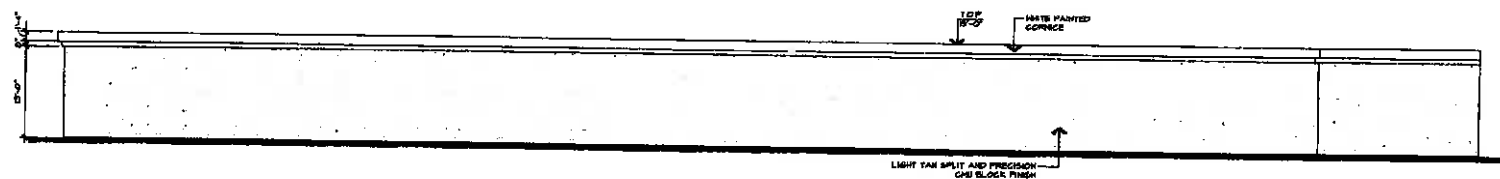
BLDG. 'B' SOUTH ELEVATION



BLDG. 'C' SOUTHWEST ELEVATION



BLDG. 'B' NORTH ELEVATION



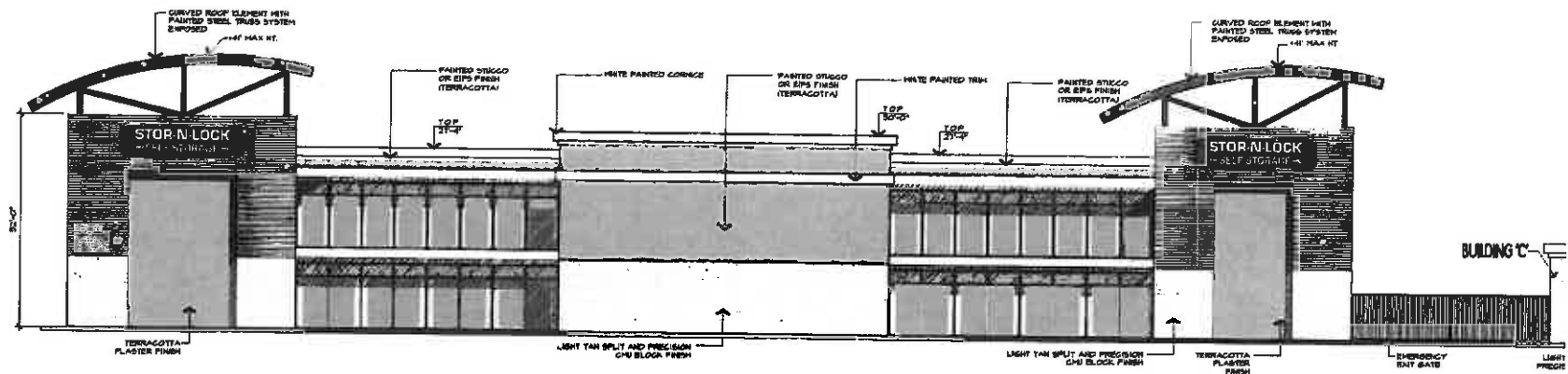
BLDG. 'C' SOUTHEAST ELEVATION

ALAN JONES
STOR-N-LOCK SELF STORAGE
 INDIO, CA

SCHEME A
PRELIMINARY BLDG. 'B' & 'C' ELEVATIONS

ALL DRAWINGS TO BE REVIEWED AND APPROVED BY THE ARCHITECT. ANY CHANGES TO BE MADE TO THESE DRAWINGS SHALL BE MADE IN WRITING AND BY THE ARCHITECT. THESE DRAWINGS SHALL BE VALID ONLY FOR THE PROJECT AND SHALL NOT BE REUSED FOR ANY OTHER PROJECT WITHOUT THE WRITTEN CONSENT OF JORDAN ARCHITECTURE, INC.
 JOB NUMBER: 14-321
 SCALE: 1/8"=1'-0"
 DATE: 10/24/2016





BLDG. 'D' SOUTHWEST ELEVATION

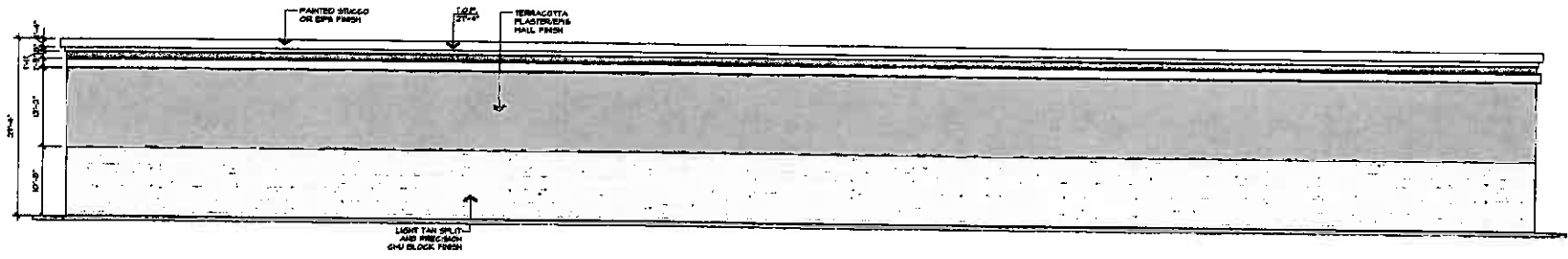
ALAN JONES
STOR-N-LOCK SELF STORAGE
 INDIO, CA

SCHEME A
PRELIMINARY BLDG. 'D' ELEVATION

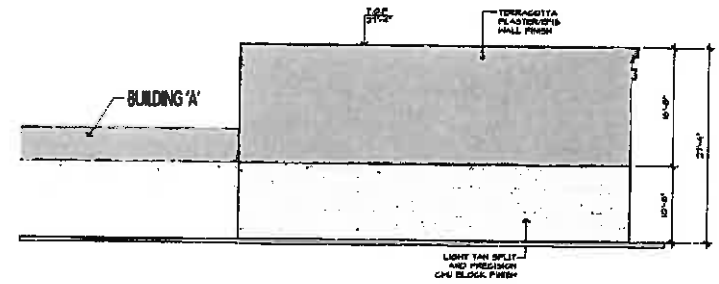
ALL RIGHTS ARE RESERVED BY ARCHITECTS, INC. FOR THIS DESIGN. NO PART OF THIS DESIGN MAY BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF ARCHITECTS, INC.

JOB NUMBER: 14-021
 SCALE: 1/8"=1'-0"
 DATE: 10/24/2016





BLDG. 'F' NORTHWEST ELEVATION



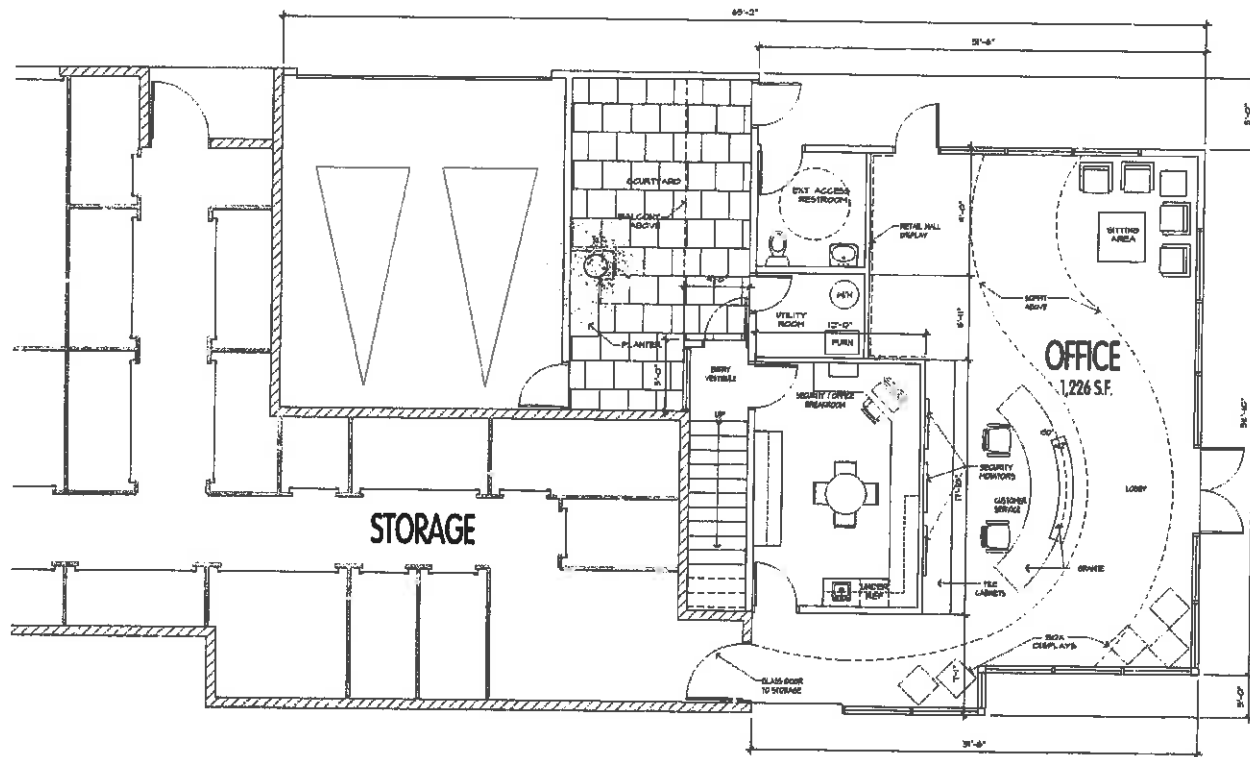
BLDG. 'F' NORTHEAST ELEVATION

ALAN JONES
STOR-N-LOCK SELF STORAGE
 INDIO, CA

SCHEME A
PRELIMINARY BLDG. 'F' ELEVATION

THIS DRAWING IS THE PROPERTY OF JORDAN ARCHITECTS, INC. NO PART OF THIS DRAWING IS TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM.
 JOB NUMBER: 16-001
 SCALE: 1/8"=1'-0"
 DATE: 10/24/2016



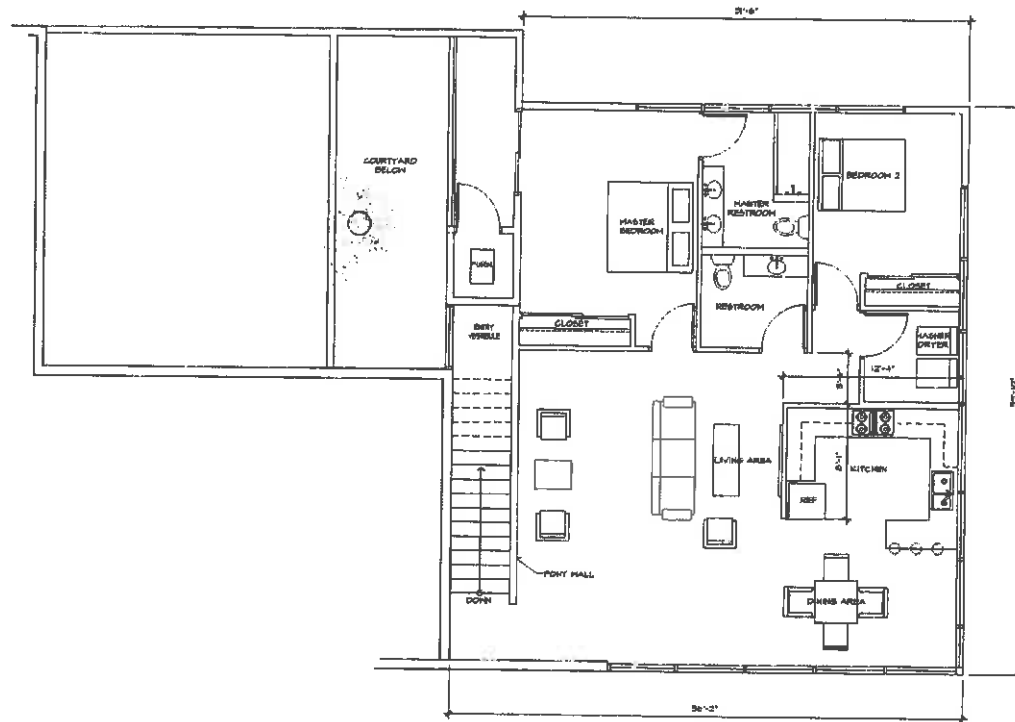


ALAN JONES
STOR-N-LOCK SELF STORAGE
 INDIO, CA

SCHEME 'H'
PRELIMINARY 1ST LEVEL FLOOR PLAN

NO PART OF THIS DOCUMENT IS TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF JORDAN ARCHITECTS, INC.
 JOB NUMBER: 16-021
 SCALE: 1/4"=1'-0"
 DATE: 12/24/2016





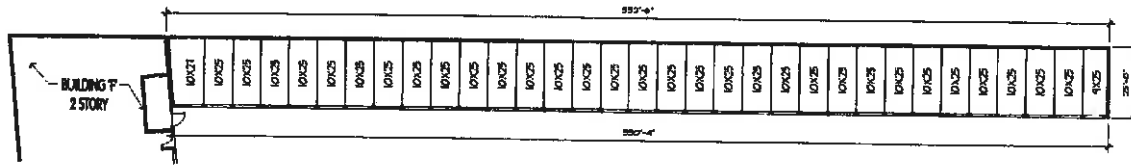
ALAN JONES
STOR-N-LOCK SELF STORAGE
 INDIO, CA

SCHEME 'H'
PRELIMINARY 2ND LEVEL RESIDENCE

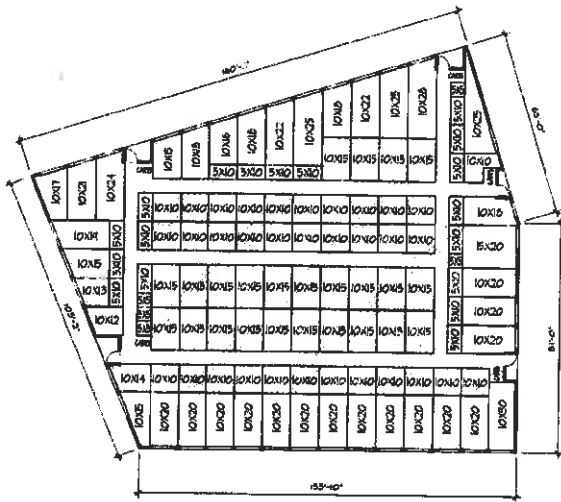
FOR REVIEW AND TO CORRECT OR AMENDMENT OF
 THIS PLAN AND TO CORRECT OR AMENDMENT OF
 ANY OTHER PLAN OR SPECIFICATION FOR THE
 PROJECT. THIS PLAN AND ANY OTHER PLAN OR
 SPECIFICATION IS VOID, OF NO FORCE OR EFFECT
 UNLESS IT IS APPROVED BY THE ARCHITECT OR
 BY THE ARCHITECT'S REPRESENTATIVE.

JOB NUMBER: 16-001
 SCALE: 1/4"=1'-0"
 DATE: 10/24/2016

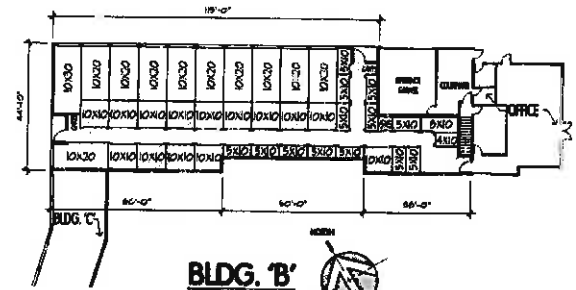




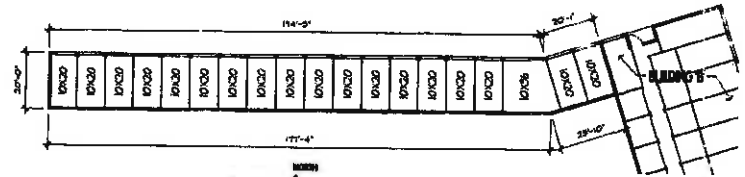
BLDG. 'A'



BLDG. 'E'



BLDG. 'B'



BLDG. 'C'

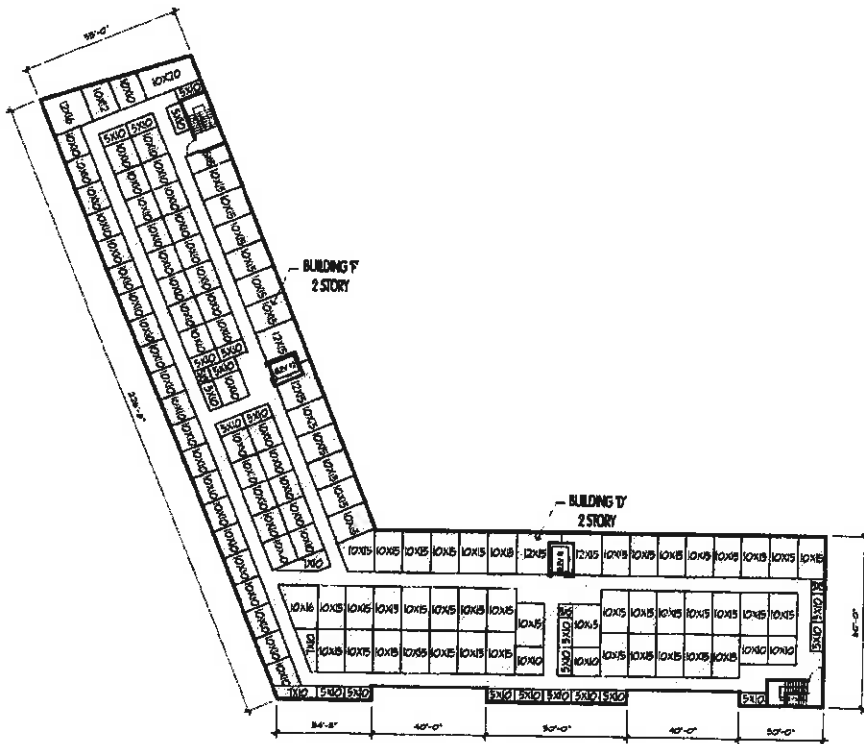
ALAN JONES
STOR-N-LOCK SELF STORAGE
 INDIO, CA

SCHEME I
PRELIMINARY BLDG. 'A', 'B', 'C' & 'E' UNIT MIX

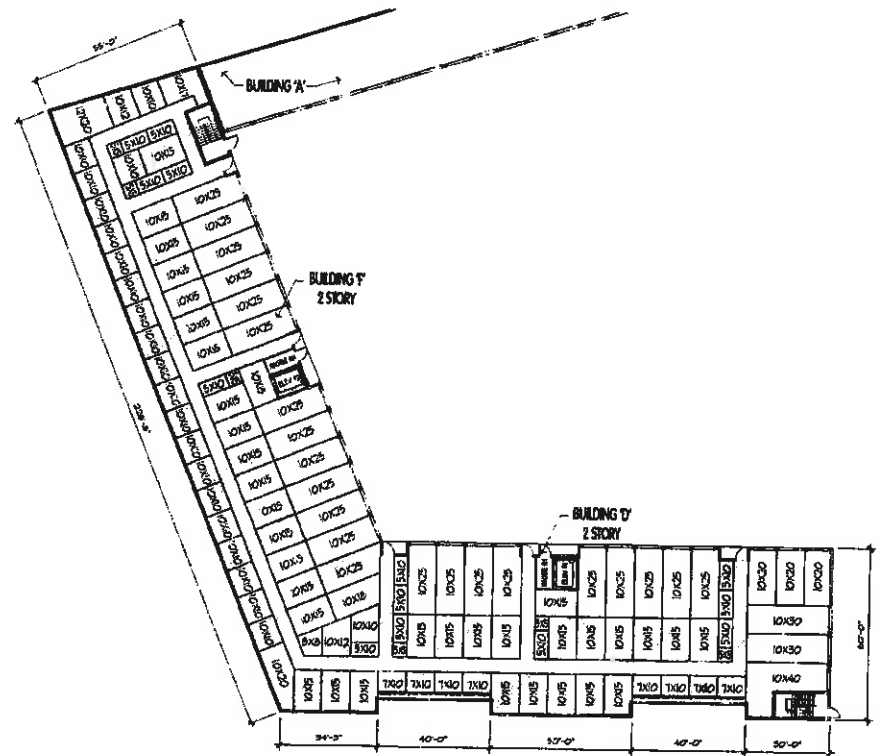
THIS DRAWING IS TO BE USED IN CONNECTION WITH THE PROJECT DESCRIBED HEREIN AND IS NOT TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF THE ARCHITECT.

JOB NUMBER: 16-021
 SCALE: 1/8" = 1'-0"
 DATE: 10/24/16





BLDG. D & F 2ND LEVEL



BLDG. 'D' & 'F' 1ST LEVEL

ALAN JONES
STOR-N-LOCK SELF STORAGE
 INDIO, CA

SCHEME H
PRELIMINARY BLDG. 'D' & 'F' UNIT MIX

SEPARATE CONTROL AREA

JOB NUMBER: 14-021
 SCALE: 1/8"=1'-0"
 DATE: 10/24/2014



NOTICE OF PUBLIC HEARING

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Thursday from 8:00 a.m. to 5:00 p.m., except Thursday, November 24 (Thanksgiving Day), and by prescheduled appointment on Friday, December 2, from 8:30 a.m. to 5:00 p.m. Please send any written comments to this address.

ATTENTION: ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. The City of Indio may hold hearings on this project and should be contacted on non-ALUC issues.

PLACE OF HEARING: Eastern Municipal Water District
2270 Trumble Road, Board Chambers
Perris, California
[Hearing venue only – do not send written comments to this address]

DATE OF HEARING: December 8, 2016

TIME OF HEARING: 9:00 A.M.

CASE DESCRIPTION:

ZAP1067BD16 – Jefferson Development 16, LLC (Representative: William Sharon, McGee Sharon Architects, Inc.) – City of Indio Planning Case Nos. CUP16-10-1019 (Conditional Use Permit) and DR16-10-404 (Design Review). A proposal to develop a commercial center with retail and restaurant buildings, convenience store with fuel pumps, and self-storage facility on 19.26 acres located along the northerly side of Indio Boulevard, easterly of its overcrossing of Interstate 10, southerly of the rail line and freeway, and westerly of Madio and Madison Streets. The project includes 50,300 square feet in five retail buildings (including a 16,200 square foot building suitable for a chain drug store pharmacy), five freestanding pads along the frontage (four with drive-thru lanes) with a cumulative floor area of 21,000 square feet, a 4,300 square foot convenience store with deli, ten gasoline pumps (20 service locations), and automated drive-thru car wash, and a seven-building 81,915 square foot self-storage facility with on-site caretaker's residence (Airport Compatibility Zones A and B1 of the Bermuda Dunes Airport Influence Area)

FURTHER INFORMATION: Contact Paul Rull at (951) 955-6893. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Mr. Mamun Faruque of the City of Indio Planning Department at (760) 341-4120.

APPLICATION FOR MAJOR LAND USE ACTION REVIEW
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

ALUC Identification No.

2AP1067BD16

PROJECT PROPONENT (TO BE COMPLETED BY APPLICANT)

Date of Application

Property Owner

JEFFERSON DEVELOPMENT 16, LLC

Phone Number

949-429-7720

Mailing Address

180 AVENIDA LA PLATA, STE. 102
SAN CLEMENTE, CA 92673

CONTACT: ED CROWDER ed@remicorporation.com

Agent (if any)

MCGEE SHARON ARCHITECTS, INC.

Phone Number

619-299-9111

Mailing Address

1530 W. LEWIS ST.
SAN DIEGO, CA 92103

CONTACT: WILLIAM SHARON wsharon@mcgeesharon.com

PROJECT LOCATION (TO BE COMPLETED BY APPLICANT)

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address

NORTH OF INDIO BLVD. BETWEEN JEFFERSON & BURN STREETS

Assessor's Parcel No.

606-080-005, 606-060-002

Parcel Size

19.26 AC

Subdivision Name

601-190-035

Lot Number

Zoning

Classification

IP

PROJECT DESCRIPTION (TO BE COMPLETED BY APPLICANT)

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use
(describe)

VACANT LAND

Proposed Land Use
(describe)

81,915 SF SELF STORAGE FACILITY, CONSISTING OF 7 BUILDINGS
71,300 SF RETAIL FACILITY, IN 10 BUILDINGS
4,300 SF C-STORE AND DELI WITH FUEL PUMPS

For Residential Uses

Number of Parcels or Units on Site (exclude secondary units)

-NA-

For Other Land Uses

Hours of Use

24 HOURS

(See Appendix C)

Number of People on Site

Maximum Number

863

Method of Calculation

575 PARKING SPACES x 1.5 OCC/CAR

Height Data

Height above Ground or Tallest Object (including antennas and trees)

38'-0" AFG

ft.

Highest Elevation (above sea level) of Any Object or Terrain on Site

82.0' ASL

ft.

Flight Hazards

Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?

Yes

No

If yes, describe

*B Dines
BI MA*

REFERRING AGENCY (APPLICANT OR JURISDICTION TO COMPLETE)	
Date Received	<u>OCT. 25, 2016</u>
Agency Name	<u>CITY OF INDIO PLANNING DEPT</u>
Staff Contact	<u>MARVIN FARVQUE</u>
Phone Number	<u>760-391-4120</u>
Agency's Project No.	<u>CUP 16-10-1019</u> <u>DR 16-10-404</u>
Type of Project	<input type="checkbox"/> General Plan Amendment <input type="checkbox"/> Zoning Amendment or Variance <input type="checkbox"/> Subdivision Approval <input type="checkbox"/> Use Permit <input type="checkbox"/> Public Facility <input checked="" type="checkbox"/> Other <u>CUP, DESIGN REVIEW</u>

EA 16-10-531

A. **NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. **SUBMISSION PACKAGE:**

ALUC REVIEW

- 1 Completed Application Form
- 1 Project Site Plan – Folded (8-1/2 x 14 max.)
- 1 Elevations of Buildings - Folded
- 1 Each . 8 ½ x 11 reduced copy of the above
- 1 8 ½ x 11 reduced copy showing project in relationship to airport.
- 1 Set . Floor plans for non-residential projects
- 4 Sets . Gummed address labels of the Owner and representative (*See Proponent*).
- 1 Set . Gummed address labels of all property owners within a 300' radius of the project site. If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10), with ALUC return address.
- 4 Sets . Gummed address labels of the referring agency (City or County).
- 1 Check for Fee (See Item "C" below)

STAFF REVIEW (Consult with ALUC staff planner as to whether project qualifies)

- 1 Completed Application Form
- 1 Project Site Plans – Folded (8-1/2 x 14 max.)
- 1 Elevations of Buildings - Folded
- 1 8 ½ x 11 Vicinity Map
- 1 Set . Gummed address labels of the Owner and representative (*See Proponent*).
- 1 Set . Gummed address labels of the referring agency.
- 1 Check for review—See Below

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.6

HEARING DATE: December 8, 2016

CASE NUMBER: ZAP1220MA16 – Rev Wheel LLC (Representative: SDH & Associates, Inc.)

APPROVING JURISDICTION: City of Riverside

JURISDICTION CASE NO: P15-1035 (Tentative Parcel Map), P16-0556 (Specific Plan Amendment), P16-0557 (Design Review)

MAJOR ISSUES: The applicant is proposing to construct predominately industrial warehouse buildings on a site located within the portion of Airport Compatibility Zone B1 that lies within Accident Potential Zone II (APZ II), as delineated by the United States Air Force in the 2005 Air Installation Compatible Use Zone (AICUZ) study. While the overall average intensity is consistent with the allowable criterion of 50 persons per acre in Compatibility Zone B1-APZ-II, Building 8 on its 1.62-acre parcel would have an average intensity of 56 people per acre, which exceeds the allowable criteria. This lot exhibits a single-acre intensity of 86 people, which is consistent with the Compatibility Plan single-acre intensity criterion for this zone (100 people per acre). However, March Air Reserve Base officials maintain that the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan is not consistent with current Air Force guidance found in Air Force Instruction 32-7063 dated December 18, 2015, which addresses Air Force policies on Land Use Compatibility in accordance with Department of Defense Instruction (DoDI) No. 4165.57. These inconsistencies include conflicts with regard to lot coverage, intensity, and permitted use definitions. The Air Force understands the DoDI as limiting intensity in APZ II to a maximum of 50 people in any given acre of building area, rather than an average intensity of 50 persons per acre of land area. Therefore, the proposed project's single acre maximum intensity of 86 people would be consistent with the ALUCP, but inconsistent with the Air Force DoDI interpretation.

RECOMMENDATION: Staff recommends that the Commission find the proposed Specific Plan Amendment CONSISTENT with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, and find the proposed Tentative Parcel Map and Design Review INCONSISTENT, due to Building 8's lot exceeding the average acre intensity for Compatibility Zone B1-APZ-II. (Additionally, single-acre intensities on most of the lots would exceed the 50-person intensity level pursuant to the Air Force interpretation.) It should be noted that reducing Building 8's mezzanine/office area from 2,500 to 1,500 square feet would bring this project into consistency with the average intensity criteria. However, such a change would not be sufficient to alleviate March Air Reserve Base concerns, based on their understanding of the Air Force and Department of Defense instructions cited above.

PROJECT DESCRIPTION: The applicant proposes to develop eight industrial buildings (predominately warehouse) ranging from 12,015 to 35,661 square feet in gross floor area, with a cumulative total of 179,710 square feet on 10.4 acres. The applicant also proposes to divide 6.5 acres located along the westerly side of Old 215 Frontage Road into six lots so that Buildings 1 through 6 will each be located on a separate lot. (Buildings 7 and 8 are already on separate lots.) The applicant also proposes to amend the Sycamore Canyon Business Park Specific Plan land use designation for these lots from Retail Business Office and Industrial Support to Industrial, and to incorporate development standards for these lots in the Specific Plan.

PROJECT LOCATION: The site is located westerly of Old 215 Frontage Road, northerly of Alessandro Boulevard, southerly of Cottonwood Avenue, and easterly of Interstate 215, within the City of Riverside, approximately 8,460 feet northwesterly of the northerly end of Runway 14-32 at March Air Reserve Base.

LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

- a. Airport Influence Area: March Air Reserve Base
- b. Land Use Policy: Zone B1-APZ II
- c. Noise Levels: 60-70 CNEL from aircraft

BACKGROUND:

Specific Plan Amendment: The change to an Industrial land use designation is appropriate for land in Accident Potential Zone II, where retail uses would likely exceed average intensity limits.

Non-Residential Average Land Use Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zone B1-APZ II. Zone B1-APZ II limits average intensity to 50 people per acre.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, the following rates were used to calculate the occupancy for the proposed project:

- Office – 1 person/200 square feet
- Warehouse – 1 person/500 square feet

The project proposes a total of 179,710 square feet of building area, which includes 15,000 square feet of office area and 164,710 square feet of warehouse area, accommodating an occupancy of 404 people, and an average intensity of 39 people, which is consistent with the Compatibility Zone B1-APZ-II criterion of 50. However, if we calculate the average intensity on a lot-by-lot basis, Building 8 results in an average intensity of 53 people per acre, which is inconsistent with the Compatibility

Zone B1-APZ-II criterion of 50. Reducing the square footage of the office/mezzanine area of Building 8 from 2,500 to 1,500 square feet would bring its average intensity down to 50 persons using the Building Code method.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle in the absence of more precise data). Based on the number of parking spaces provided (251 spaces), the total occupancy would be estimated at 377 people for an average acre intensity of 36 people per acre, which is consistent with the Compatibility Zone B1-APZ-II criterion of 50. However, if we calculate the average intensity on a lot-by-lot basis, the lot including Building 8 includes 60 parking stalls, resulting in 90 people and an average acre intensity of 56 people per acre, which is inconsistent with the Compatibility Zone B1-APZ-II criterion of 50. Vehicle occupancy for uses of this type that do not provide direct services to the public may be closer to 1.25 persons per vehicle, which would result in an occupancy of 75 people and an average intensity of 46 people per acre.

Based on the lot sizes of the existing and proposed parcels, the individual lot intensity limits on the basis of 50 persons per acre of land area would be as follows: 82, 50, 31, 33, 62, 77, 101, and 81 persons for Buildings 1 through 8, respectively.

Non-Residential Single-Acre Land Use Intensity: Compatibility Zone B1-APZ-II limits maximum single-acre intensity to 100 people. There are no risk-reduction design bonuses available, as March Air Reserve Base/Inland Port Airport is primarily utilized by large aircraft weighing more than 12,500 pounds.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre area occurs at Building 8 consisting of 30,661 square feet of warehouse area, 2,500 square feet of first floor office area, and 2,500 square feet of second floor office/mezzanine area for a total occupancy of 86 people, which is consistent with Compatibility Zone B1-APZ-II single acre intensity criterion of 100. However, given that Building 8 is on a parcel of 1.62 acres, 86 would exceed the parcel-based average intensity limit of 81 people. Furthermore, ongoing communication with the Air Force indicates that the single-acre intensity within APZs should not exceed the allowable average intensity of 50 people.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zone B1-APZ II. Industrial warehouse buildings are compatible within Accident Potential Zone II pursuant to the 2005 Air Installation Compatible Use Zone (AICUZ) study disseminated by the United States Air Force. Use as an industrial warehouse is also compatible pursuant to subsequent (2011) Department of Defense Instruction (DODI) No. 4165.57, but the intensity levels of this project exceed DODI allowances, as understood by the Air Force.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being in an area between 60-70 CNEL range from aircraft noise. As a primarily industrial use

not sensitive to noise (and considering typical anticipated building construction noise attenuation of approximately 20 dBA), the warehouse area would not require special measures to mitigate aircraft-generated noise. However, a condition is included to provide for adequate noise attenuation within office areas of the building.

Part 77: The elevation of Runway 14-32 at its northerly terminus is 1,535 feet above mean sea level (1,535 feet AMSL). At a distance of approximately 8,460 feet from the runway to the closest parcel within the site, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,619.6 feet AMSL. The site elevation is 1,539 feet AMSL. With a maximum building height of 33 feet, the top point elevation would be 1,572 feet AMSL. Therefore, review of buildings by the FAA Obstruction Evaluation Service is not required.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically. However, development within Compatibility Zone B1-APZ-II is limited to maximum lot coverage of 50%. The proposed project will have a total lot coverage of approximately 40% (building 3 has the highest lot coverage of 44%), which is consistent with Zone B1-APZ-II maximum lot coverage of 50%.

CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible

wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)

- (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children’s schools, day care centers, libraries, hospitals, skilled nursing and care facilities, congregate care facilities, hotels/motels, restaurants, places of assembly (including churches and theaters), buildings with more than 3 aboveground habitable floors, noise sensitive outdoor nonresidential uses, critical community infrastructure facilities and hazards to flight.
3. Prior to issuance of any building permits, the landowner shall convey and have recorded an avigation easement to the March Inland Port Airport Authority. Contact March Joint Powers Authority at (951) 656-7000 for additional information.
 4. The attached notice shall be given to all prospective purchasers of the property and tenants of the building.
 5. Any proposed detention basins on the site (including water quality management basins) shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the detention basin(s) shall not include trees that produce seeds, fruits, or berries.
 6. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
 7. Noise attenuation measures shall be incorporated into the design of the office areas of the structure, to the extent such measures are necessary to ensure that interior noise levels from aircraft operations are at or below 45 CNEL.
 8. Occupancy of these structures shall be limited as follows:

Building 1	82
Building 2	50
Building 3	31

Staff Report

Page 6 of 6

Building 4	33
Building 5	62
Building 6	77
Building 7	101
Building 8	81

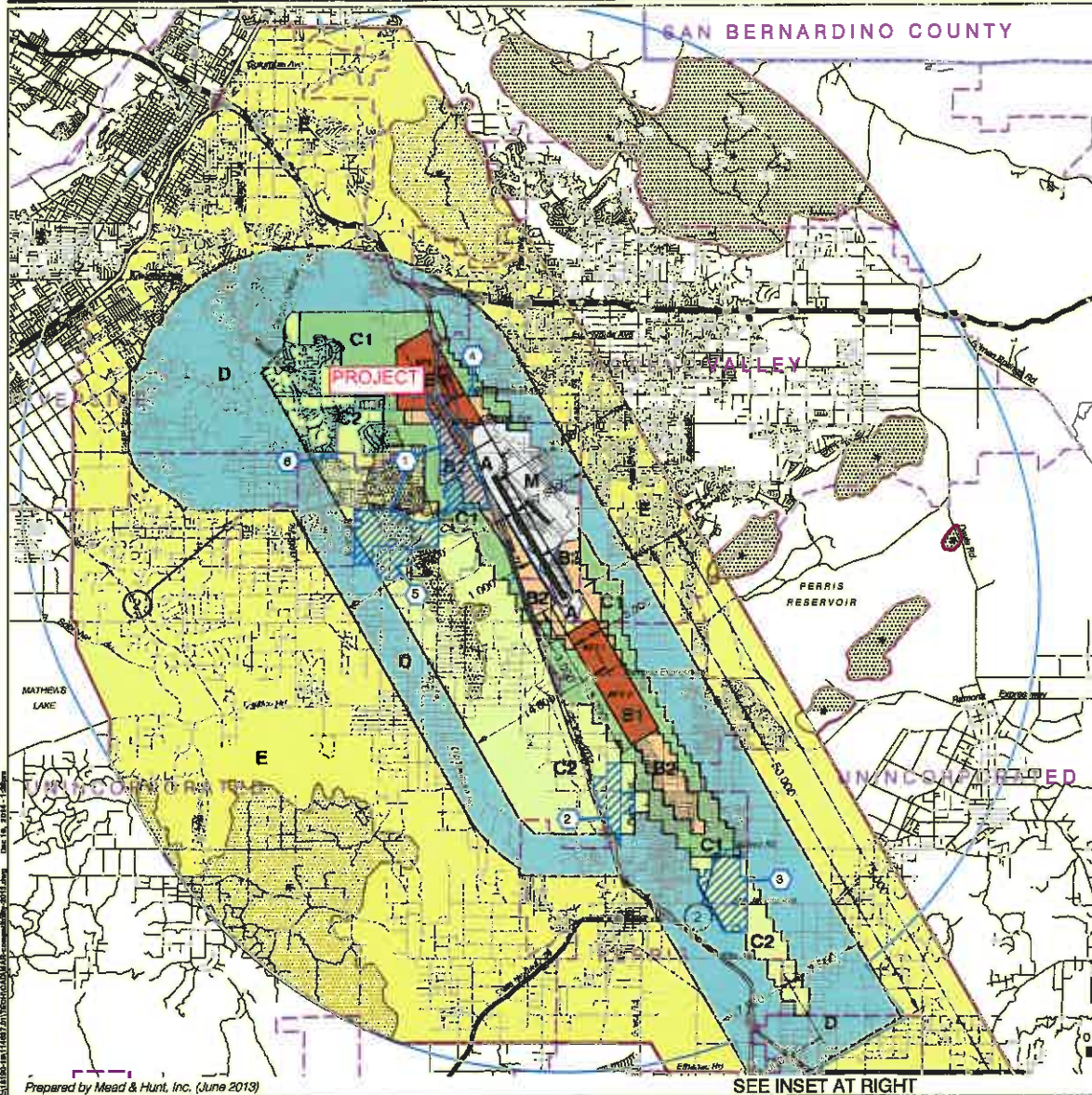
Occupancy of these structures shall be limited to assure that none of the lots exceed an intensity of 50 persons per acre of land area.

9. This project has been evaluated for 15,000 square feet of office area and 164,710 square feet of warehouse area. Any increase in building area or change in use will require review by the Airport Land Use Commission. In addition, this project shall not store, process or manufacture hazardous materials without review and approval by the Airport Land Use Commission.
10. Uses in Building 8 shall not provide on-site services to the public.

Y:\AIRPORT CASE FILES\March\ZAP1220MA16\ZAP1220MA16sr.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



LEGEND

Compatibility Zones

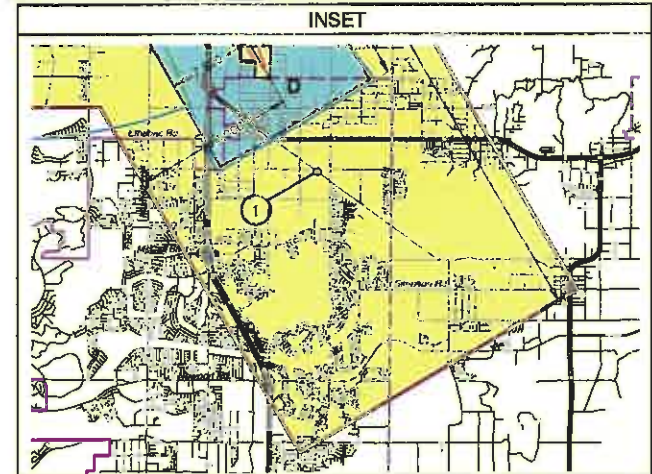
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

- 1 Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,595 feet MSL.
- 2 Point at which departing aircraft typically reach 3,000 feet above runway end.

- 1 March JPA: March Business Center/Meridian
- 2 Perris: Harvest Landing
- 3 Perris: Park West
- 4 Moreno Valley: Affordable Housing
- 5 March JPA: Ben Clark Training Center
- 6 Riverside: Ridge Crest Subdivision



Note:
All dimensions are measured from runway ends and centerlines.



Base map source: County of Riverside 2013

Prepared by Mead & Hunt, Inc. (June 2013)

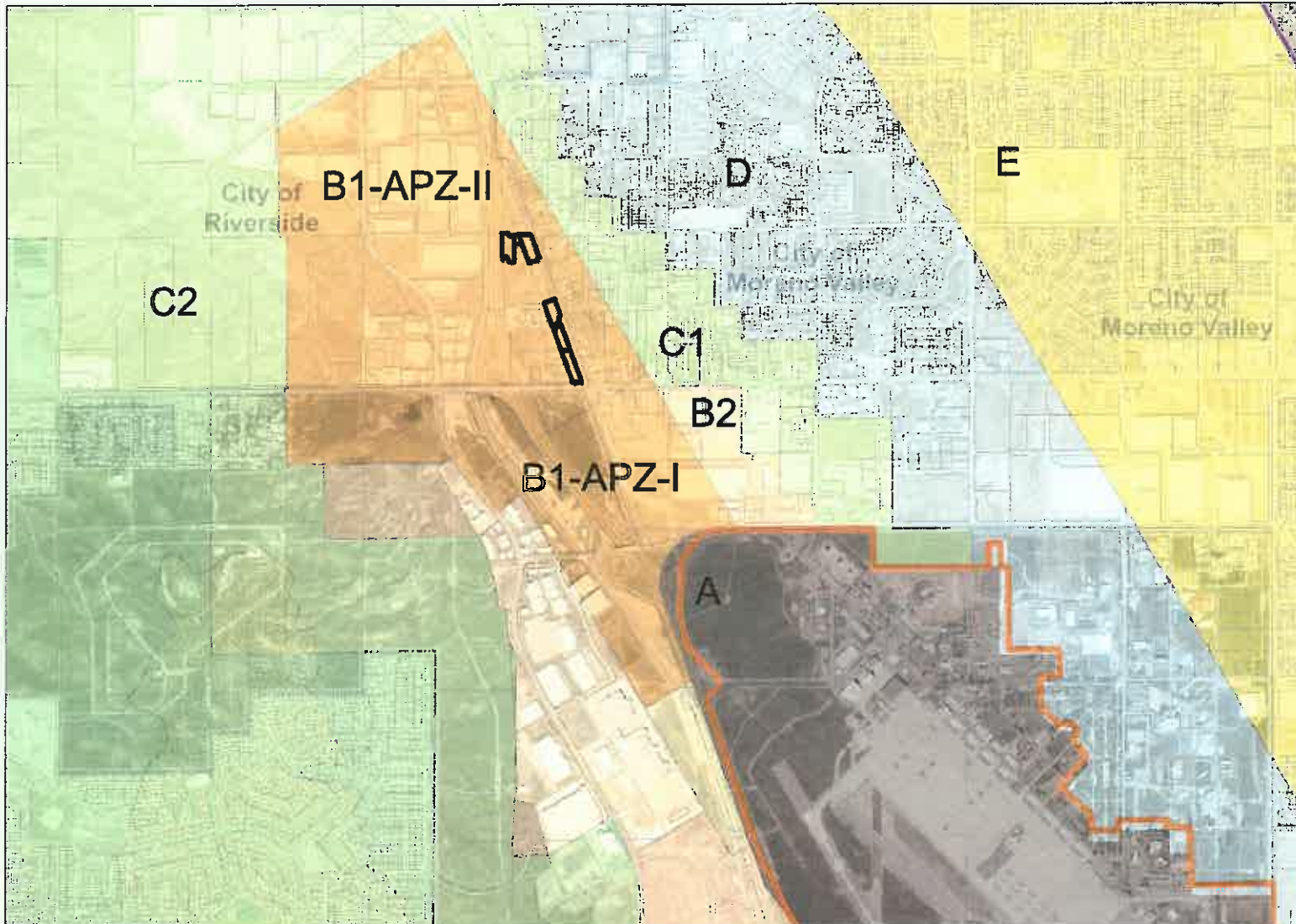
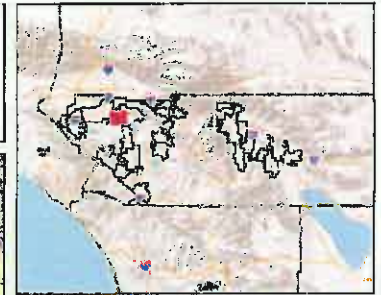
SEE INSET AT RIGHT

Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)

Map MA-1

Compatibility Map
March Air Reserve Base / Inland Port Airport

My Map



Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

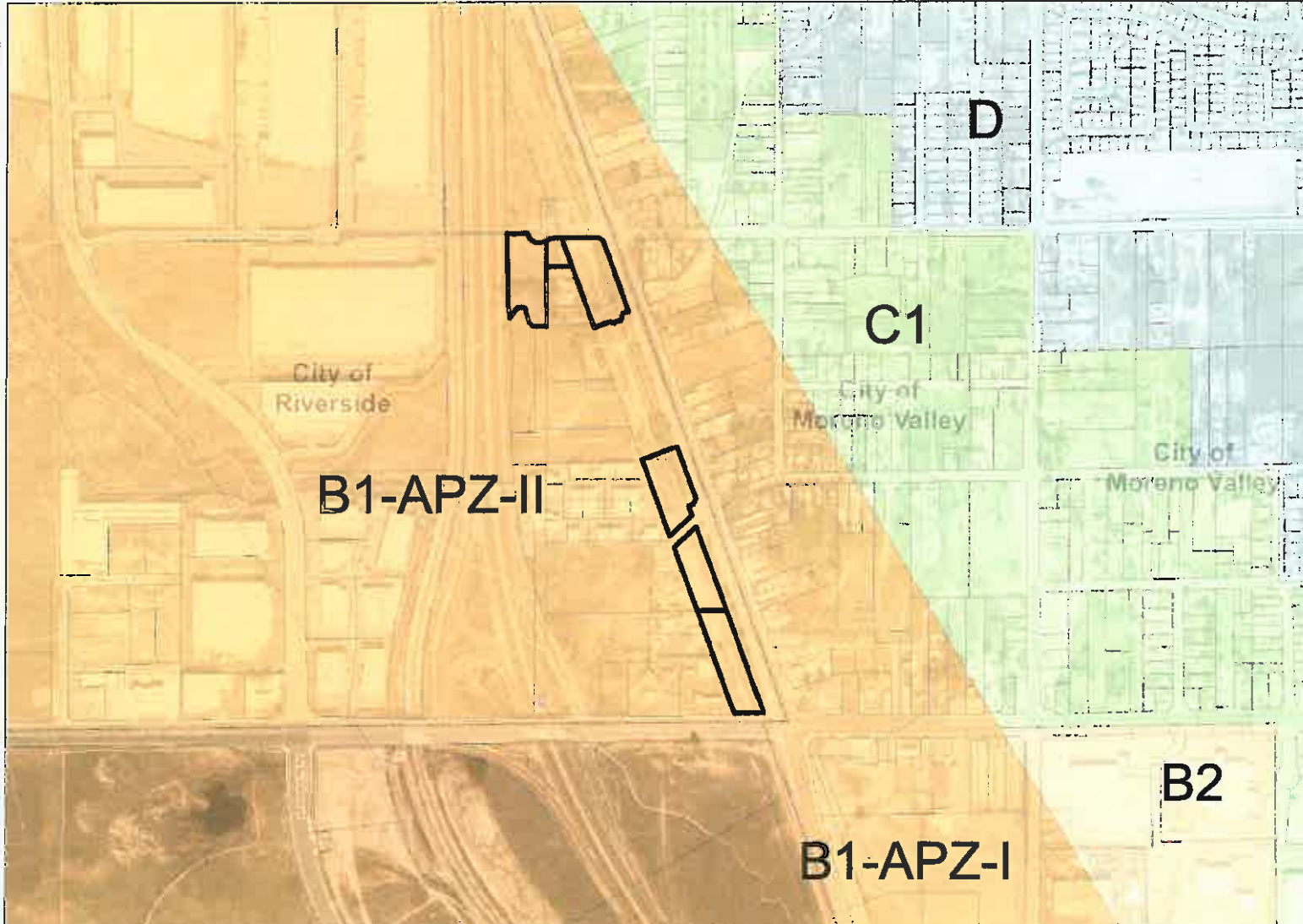
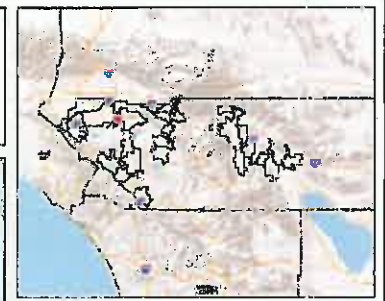


REPORT PRINTED ON... 11/16/2016 3:01:09 PM

© Riverside County RCIT GIS

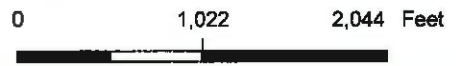
Notes

My Map



Legend

- Display Parcels
- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC8



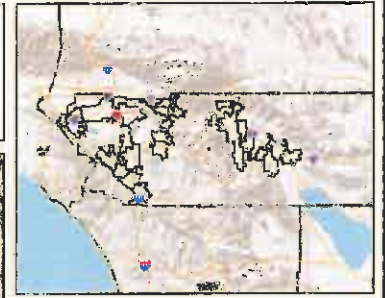
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/16/2016 2:39:48 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities**
- roadsanno**
- highways**
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines**
- waterbodies**
- Lakes
- Rivers



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

0 1,022 2,044 Feet

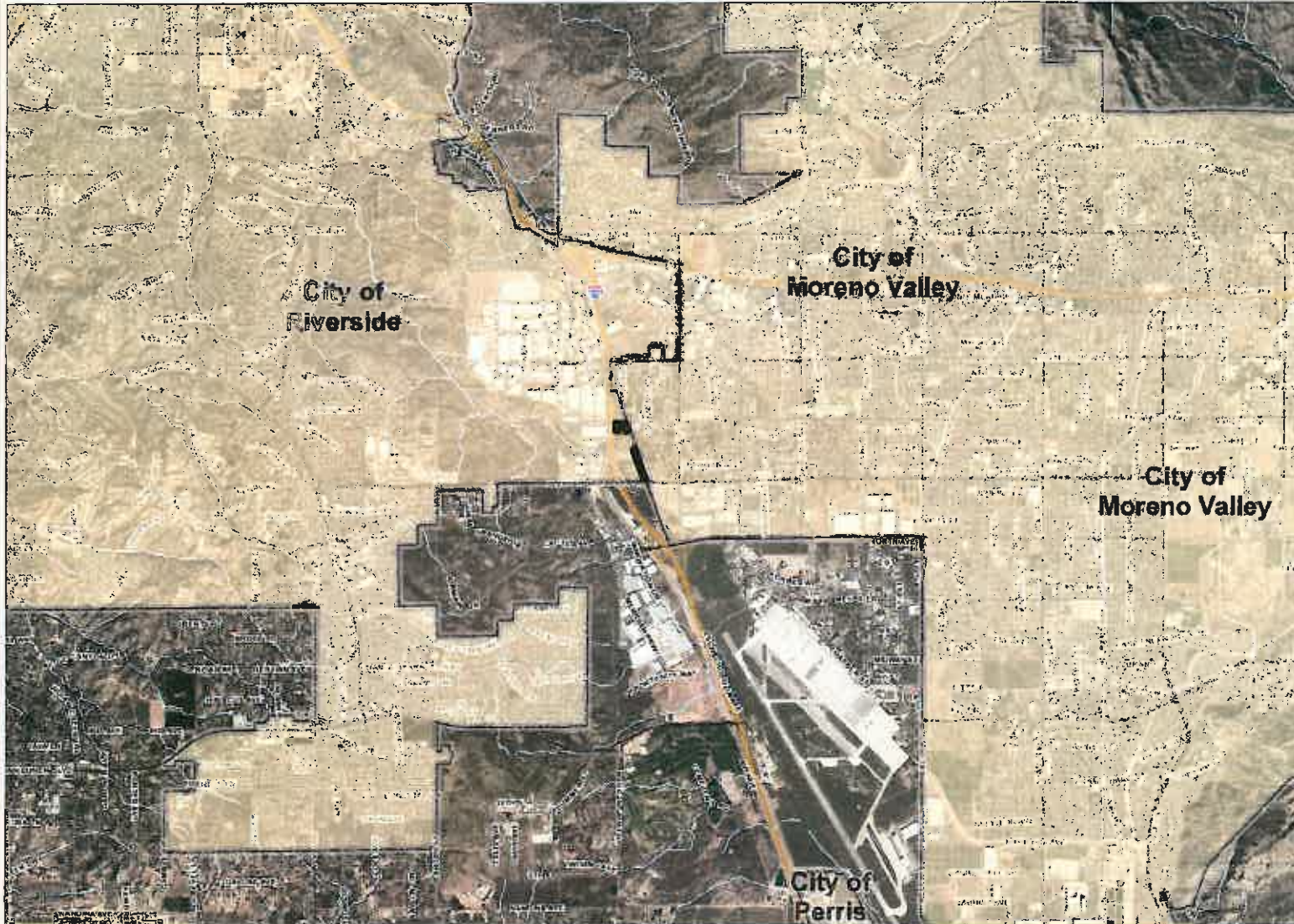
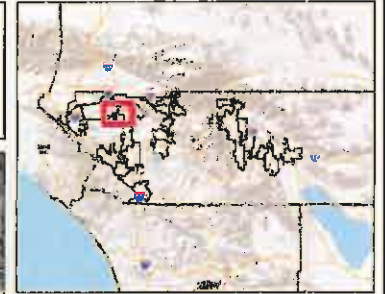


REPORT PRINTED ON...11/16/2016 2:41:04 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- City Boundaries
- Cities
- highways_large**
 - HWY
 - INTERCHANGE
 - INTERSTATE
 - USHWY
- majorroads
- counties
- cities

Notes



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

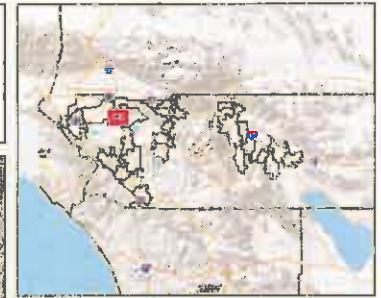
0 8,177 16,355 Feet



REPORT PRINTED ON... 11/16/2016 2:57:12 PM

© Riverside County RCIT GIS

My Map



Legend

- City Boundaries
- Cities
- roads**
- sanno**
- highways**
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads**
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities
- hydrography**lines
- waterbodies**
- Lakes
- Rivers



0 4,089 8,177 Feet



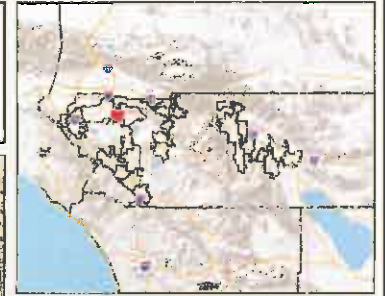
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/16/2016 2:57:46 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- City Boundaries
- Cities
- roadsanno**
- highways**
 - HWY
 - INTERCHANGE
 - INTERSTATE
 - OFFRAMP
 - ONRAMP
 - USHWY
- roads**
 - Major Roads
 - Arterial
 - Collector
 - Residential
- counties
- cities
- hydrographylines**
- waterbodies**
 - Lakes
 - Rivers



0 2,044 4,089 Feet



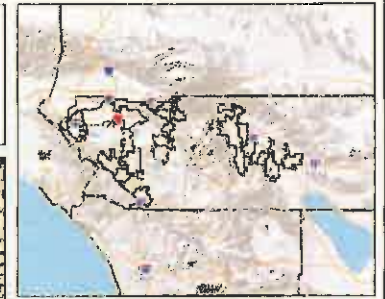
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/16/2016 2:58:19 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities**
- roadsanno**
- highways**
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines**
- waterbodies**
- Lakes
- Rivers



0 1,022 2,044 Feet



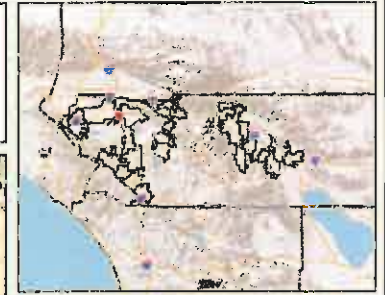
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/16/2016 2:58:44 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities
- roadsanno**
- highways**
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines**
- waterbodies**
- Lakes
- Rivers



0 818 1,635 Feet



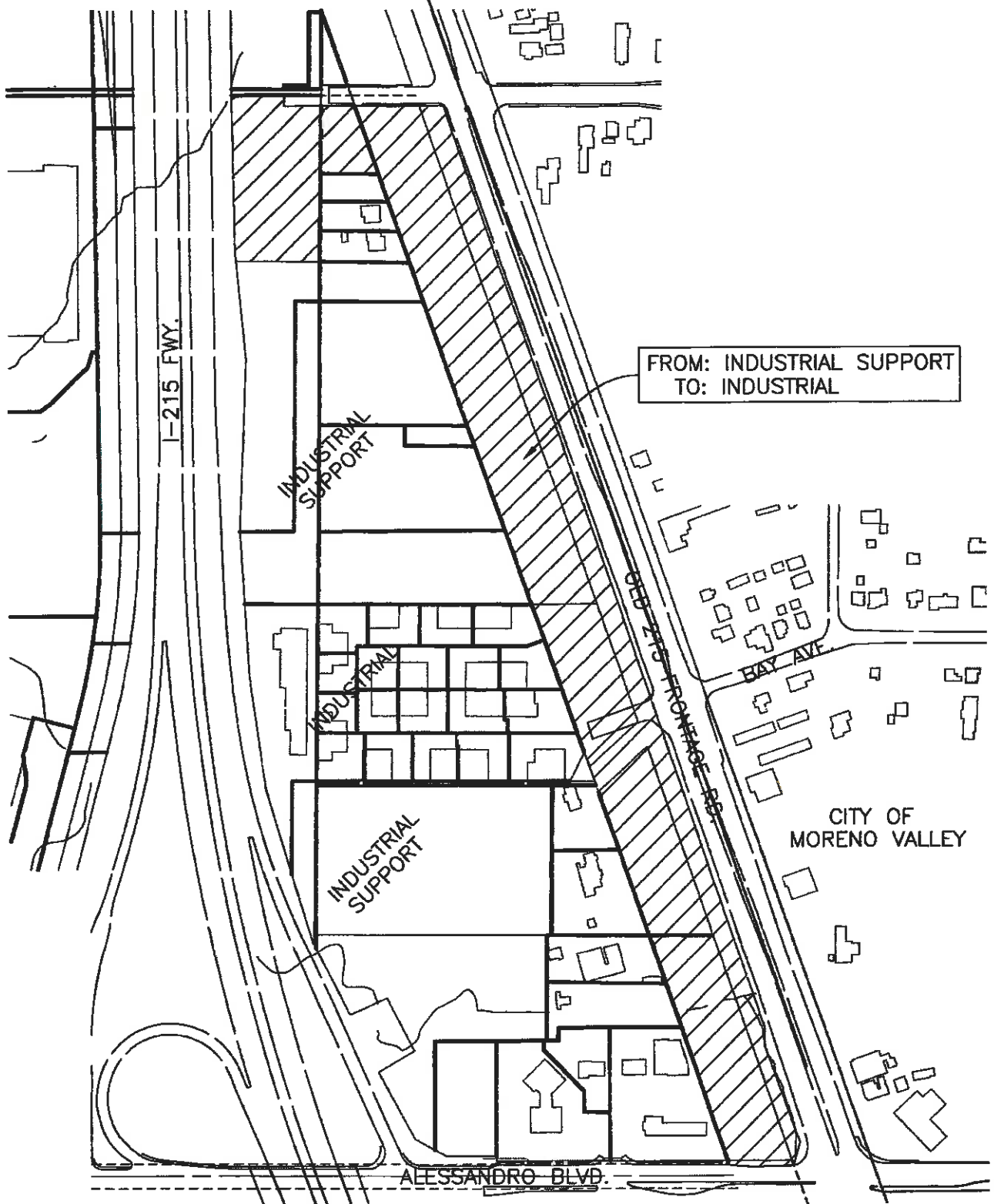
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/16/2016 2:59:30 PM

© Riverside County RCIT GIS

Notes

LAND USE DESIGNATION



1"=350'

PREPARED BY:		SDH AND ASSOCIATES INC. 5225 CANYON CREST DRIVE 71439 Riverside, California 92507 TEL: (951) 683-3691 FAX (951) 788-2314
CITY OF RIVERSIDE		1 OF 1 SHEETS
EXHIBIT "A"		
LAND USE DESIGNATION		

SPECIFIC PLAN SETBACK AMENDMENT

PROJECT DESCRIPTION

The proposed development consists of two non-contiguous land areas along the westerly side of Old 215 Frontage Road between Alessandro Boulevard and Cottonwood Avenue. The southerly portion is reflected in Tentative Parcel Map No. 36981, which is a proposal to subdivide approximately 6.5 acres into six parcels ranging from 27,105 sf to 72,413 (gross) sf in size. The northerly portion consists of Parcels 3 and 4 of Parcel Map 36888, which was processed through the City of Riverside and recorded on March 1, 2016. Parcel 3 is 70,714 SF and Parcel 4 is 88,723 SF.

Access to Buildings 1 through 6 will be provided via driveways along Old 215 Frontage Road and Alessandro Boulevard, along with the (private) extension of Bay Street which will also allow access to PM 32297 (which currently does not have legal access). Access to Buildings 7 and 8 will be provided by driveways on Cottonwood Avenue. Building 7 will also be served by a driveway on Old 215 Frontage Road.

PROPOSED SPECIFIC PLAN AMENDMENT

Insert Paragraph C, below to follow paragraph B, of Section 3.2.1 "Setback Standards" of the Sycamore Canyon Business Park Specific Plan, as previously amended by Resolution No. 20437:

"C. For parcels abutting Old 215 Frontage Road between Alessandro Boulevard and Cottonwood Avenue where the building is less than 30-feet in height; a front yard having a depth of no less than 10 feet. The entirety of which shall be suitably landscaped with plant materials and adequately maintained. Notwithstanding any other provisions of this Title to the contrary, pedestrian walks and vehicular access drives, which are perpendicular to the front property line within the front ten-feet, meter pits, and utility manholes extending not more than six inches above finished grade, light fixtures and any recording instruments required by this Chapter shall be permitted in any front yard."

REQUIRED FINDINGS

Will the strict application of the provisions of the Zoning Code result in practical difficulties or unnecessary hardships inconsistent with the general purpose and intent of the Zoning Code?

- 1) Yes. The existing property is comprised of remnant Rights-of-Way; a portion of the Old 215 highway right of way and the vacated AT & SF Railroad right of way. The resulting property is linear and very limited in depth.
 - a. The property boundary is a remnant of unused Old 215 right of way and AT & SF right of way which limits its depth. Further, existing adjacent developments (as well as other existing developments in the immediate area) have established type of building/development that will be the typical for this area. Existing RPU and Edison electric lines, City sewer and the Kinder Morgan liquid petroleum line further limit the buildable area of the site.

- b. Various site layouts were explored. However, the need for clear access over the utility lines and access to the existing properties westerly of the project, drove the direction of the layout as eight individual buildings with reduced front yard setbacks.
- c. The project design and lot layout were dictated in many ways by the irregular (narrow) shape of the parcels. As well, the established (typical) development of the area also dictated the building size and shape. Building layouts, widths and depths all needed to be considered due to this constraining land feature that is not present throughout most of the area.

Are there special circumstances or conditions applicable to your property or to the intended use or development of your property that do not generally apply to other property in the vicinity and under the identical zoning classification?

- 2) Yes. Special circumstances, such as the property's unusual shape, significantly constrain the way the parcels can be configured. This limits the configuration and location of the proposed buildings.
 - a. The narrow remnants of abandoned highway and railroad rights-of-way coupled with the encroachment of existing utilities at the rear of the property, limit the depth of the proposed buildings.
 - b. Internal access, along with access to the existing parcels westerly of the project, need to be maintained along the westerly portion of the subject property, which may be further limit the configuration and location of the buildings.

Will the granting of such variance prove materially detrimental to the public welfare or injurious to the property or improvements in the zone or neighborhood in which your property is located?

- 3) No. The granting of the variances will allow a clear easement for existing utilities and access to the existing properties to the west.
 - a. Electric, gas, Kinder Morgan and the City of Riverside have facilities along the westerly edge of the subject property. The proposed project will establish proper easements and access for these utilities.
 - b. The properties westerly of the proposed project do not currently have legal access to the public street. The project will benefit the existing properties by establishing an access easement along the westerly boundary of the proposed project.

Will the granting of such a variance be contrary to the objectives of any part of the General Plan?

- 4) No. The variances requested for individual lot areas will allow the lots to have a size and shape that are similar to the adjacent developed parcels and will provide for the design of building footprints that are in the same character as the existing developments in the immediate area. The proposed project lies east and south of an established commercial/industrial area.

SPECIFIC PLAN

LOT AREA/LOT WIDTH AMENDMENT

PROJECT DESCRIPTION

The proposed development consists of two non-contiguous land areas along the westerly side of Old 215 Frontage Road between Alessandro Boulevard and Cottonwood Avenue. The southerly portion is reflected in Tentative Parcel Map No. 36981, which is a proposal to subdivide approximately 6.5 acres into six parcels ranging from 27,105 sf to 72,413 (gross) sf in size. The northerly portion consists of Parcels 3 and 4 of Parcel Map 36888, which was processed through the City of Riverside and recorded on March 1, 2016. Parcel 3 is 70,714 SF and Parcel 4 is 88,723 SF.

Access to Buildings 1 through 6 will be provided via driveways along Old 215 Frontage Road and Alessandro Boulevard, along with the (private) extension of Bay Street which will also allow access to PM 32297 (which currently does not have legal access). Access to Buildings 7 and 8 will be provided by driveways on Cottonwood Avenue. Building 7 will also be served by a driveway on Old 215 Frontage Road.

PROPOSED SPECIFIC PLAN AMENDMENT

Insert the paragraph below to follow paragraph 2, on page 30, of Section 3.2 "Lot Standards" of the Sycamore Canyon Business Park Specific Plan:

"A minimum lot size of ½ acre is allowable for that portion of land within Annexation 104-"Old Highway 215 Corridor" which is located along Old 215 Frontage Road between Alessandro Boulevard and Cottonwood Avenue. This area of land previously served as the Atchison, Topeka & Santa Fe Railroad Right-of-Way and a portion of the State Highway 215 Right-of-Way. Due to the unusually narrow configuration of the land, ½ acre minimum lot size will allow for logical development that still meets the goals of the Sycamore Canyon Business Park Specific Plan."

Revise the last sentence on page 30 of Section 3.2 as follows:

"Minimum lot widths shall be 300 feet for five acre and larger lots, 140 feet for one acre lots and 125 feet for ½ acre lots."

REQUIRED FINDINGS

Will the strict application of the provisions of the Zoning Code result in practical difficulties or unnecessary hardships inconsistent with the general purpose and intent of the Zoning Code?

- 1) Yes. The existing property is comprised of remnant Rights-of-Way; a portion of the Old 215 highway right of way and the vacated AT & SF Railroad right of way. The resulting property is linear and very limited in depth.
 - a. The property boundary is a remnant of unused Old 215 right of way and AT & SF right of way which limits its depth. Further, existing adjacent developments (as well as other existing developments in the immediate area) have established type of building/development that will be the typical for this area. Further, existing RPU and Edison electric lines, City sewer and the Kinder Morgan liquid petroleum line limit the buildable area of the site.

- b. Various site layouts were explored. However, the need for clear access over the utility lines and access to the existing properties westerly of the project, drove the direction of the layout as eight individual buildings.
- c. The project design and lot layout were dictated in many ways by the irregular (narrow) shape of the parcels. As well, the established (typical) development of the area also dictated the building size and shape. Building layouts, widths and depths all needed to be considered due to this constraining land feature that is not present throughout most of the area.

Are there special circumstances or conditions applicable to your property or to the intended use or development of your property that do not generally apply to other property in the vicinity and under the identical zoning classification?

- 2) Yes. Special circumstances, such as the property's unusual shape, significantly constrain the way the parcels can be configured. This limits the configuration of the proposed buildings, which ultimately affects the size of the parcels.
 - a. The narrow remnants of abandoned highway and railroad rights-of-way coupled with the encroachment of existing utilities at the rear of the property, limit the depth of the proposed buildings.
 - b. Access to the existing parcels westerly of the project need to be maintained along the westerly portion of the subject property, which may be further limit the depth of the buildings.

Will the granting of such variance prove materially detrimental to the public welfare or injurious to the property or improvements in the zone or neighborhood in which your property is located?

- 3) No. The granting of the variances will allow a clear easement for existing utilities and access to the existing properties to the west.
 - a. Electric, gas, Kinder Morgan and the City of Riverside have facilities along the westerly edge of the subject property. The proposed project will establish proper easements and access for these utilities.
 - b. The properties westerly of the proposed project do not currently have legal access to the public street. The project will benefit the existing properties by establishing an access easement along the westerly boundary of the proposed project.

Will the granting of such a variance be contrary to the objectives of any part of the General Plan?

- 4) No. The variances requested for individual lot areas will allow the lots to have a size and shape that are similar to the adjacent developed parcels and will provide for the design of building footprints that are in the same character as the existing developments in the immediate area. The proposed project lies east and south of an established commercial/industrial area.

IN THE CITY RIVERSIDE, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA.
TENTATIVE PARCEL MAP NO. 36981

PORT. SEC. 10, T.2S., R.4W., S.B.B.M.

AUGUST 2016

UNDERGROUND STRUCTURES

1. NOTHING FOUND, NOTHING SET AT ALL PROPERTY CORNERS UNLESS OTHERWISE NOTED.
2. PURSUANT TO SECTION 8709 OF THE PROFESSIONAL LAND SURVEYORS ACT IN CONFORMITY WITH THE PRACTICE OF LAND SURVEYING, ANY PARCEL SHALL REQUIRE THE FILING OF A RECORD OF SURVEY OR CORNER RECORD.
3. TO THE BEST OF OUR KNOWLEDGE ALL EVIDENCE OF ABOVE GROUND UTILITIES ARE SHOWN HEREON. ANY UNDERGROUND UTILITIES ARE OBTAINED BY SEARCH OF AVAILABLE RECORDS. THE UTILITIES SERVING THE SUBJECT PROPERTY APPEAR TO ENTER VIA A PUBLIC RIGHT-OF-WAY.



VICINITY MAP
 NOT TO SCALE
 THOMAS GUIDE PAGE: 716 GRID: J5

APPLICANT/OWNER

REV. WHEEL, LLC
 ATTN: BOB MCNATH
 911 EAGLE CREST
 RIVERSIDE, CA 92506

ENGINEER

SM & ASSOCIATES, INC.
 6236 CANYON CREST DRIVE 71438
 RIVERSIDE, CA 92507
 VOICE: (951) 683-3691
 FAX: (951) 788-2314

SOILS ENGINEER

WORLD ENGINEERING
 10841 HANCOCK ST.
 LOS ANGELES, CA 90020
 VOICE: (323) 790-9460
 FAX: (323) 798-9456

SURVEY

GROUND SURVEY BY:
 WALSTON LAND SURVEYING
 26346 ROAN RANCH RD.
 RIVERSIDE, CA 92506
 VOICE: (951) 878-9934

LEGAL DESCRIPTION

BEING A DIVISION OF A PORTION OF THE SOUTHEAST QUARTER AND THE SOUTHWEST QUARTER OF SECTION 19, TOWNSHIP 1 SOUTH, RANGES 4 WEST, SAN BERNARDINO SAN BERNARDINO MERIDIAN APPROX. AREA 5.07 AC.

ASSESSOR'S PARCEL NO.

263-081-015 & 263-100-021

UTILITY PURVEYORS

WATER.....NAYO
 GAS.....SD, CALIF. GAS
 ELECTRICAL.....CITY OF RIVERSIDE/ SD, CAL. EDISON
 TELEPHONE.....PACIFIC BELL
 CABLE.....CITY OF RIVERSIDE
 FIBER.....CHARTER COMMUNICATION

SCHOOL DISTRICT

RIVERSIDE UNIFIED
 SCHOOL DISTRICT

ZONING AND LAND USE

EXISTING ZONING - BMP
 PROPOSED ZONING - BMP

SURROUNDING LAND USE

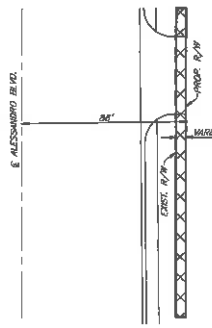
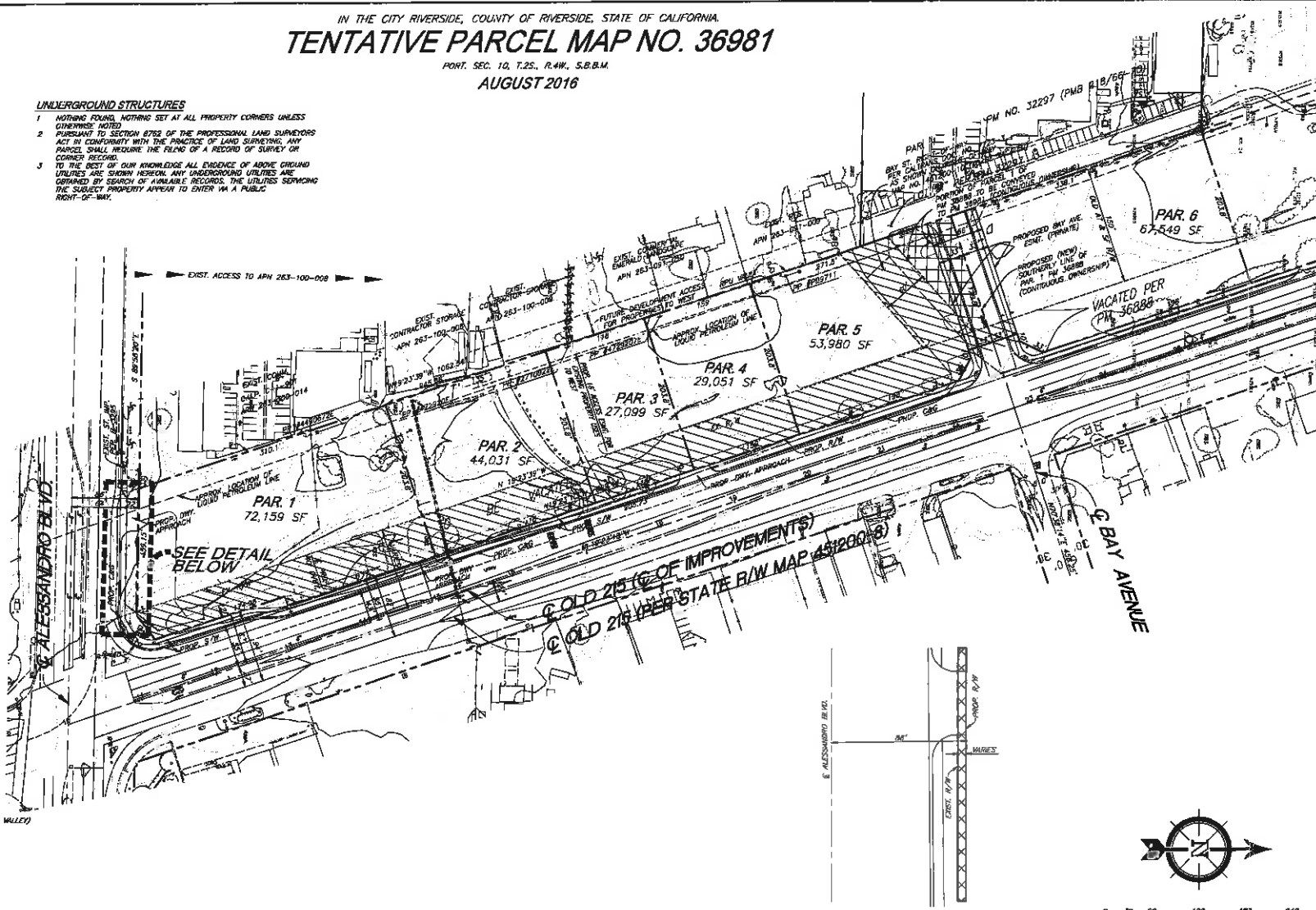
NORTH.....INDUST.
 EAST.....RESIDENTIAL/COMMERCIAL/VACANT (CITY OF MORENO VALLEY)
 SOUTH.....VACANT
 WEST.....COMMERCIAL/RESIDENTIAL

SURROUNDING ZONING

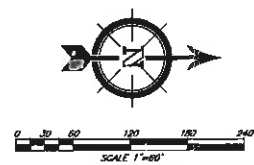
NORTH.....BMP
 EAST.....N/A - MORENO VALLEY
 SOUTH.....BMP
 WEST.....BMP

LEGEND

- PROP. PARCEL LINE
- EXIST. PARCEL LINE
- CENTERLINE
- EX. R/W (TO BE VACATED)=66,997 S.F.
- PROP. R/W (TO BE DEDICATED)=248 S.F.

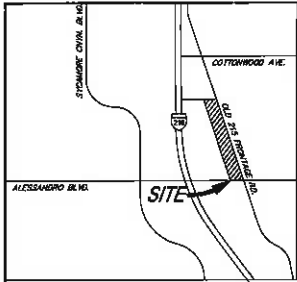


ALESSANDRO R/W DEDICATION
 NOT TO SCALE



	PREPARED BY: 	8211 AND ASSOCIATES, INC. 6236 CANYON CREST DRIVE 71438 RIVERSIDE, CALIFORNIA 92507 TEL: (951) 683-3691 FAX: (951) 788-2314	BUSINESS DAY CERTIFICATE NO. 107204 EXP. 9-13-17 CITY OF RIVERSIDE TENTATIVE PARCEL MAP 36981 OLD 215 INDUSTRIAL	1 OF 1 SHEETS
	SCALE: 1"=60' DATE: AUGUST 2016	BENCHMARK: 1985 B.M. 114.114 - established 1985 A 1"=60' SCALE PLAN OF A PORTION OF THE 6-AC INDUSTRIAL AND COMMERCIAL TRACT OF 215 INDUSTRIAL, RANGES 4 WEST, SAN BERNARDINO MERIDIAN, TOWNSHIP 1 SOUTH, RANGES 4 WEST, SAN BERNARDINO COUNTY, CALIFORNIA, SHOWING THE PROPOSED DEDICATION OF A PORTION OF THE TRACT TO THE CITY OF RIVERSIDE FOR THE PURPOSES OF THE PROPOSED OLD 215 INDUSTRIAL TRACT. THE CITY OF RIVERSIDE HAS REVIEWED THIS PLAN AND HAS FOUND IT TO BE IN ACCORDANCE WITH THE CITY OF RIVERSIDE PLANNING AND ZONING ORDINANCES AND THE CITY OF RIVERSIDE PLANNING AND ZONING COMMISSION HAS APPROVED THIS PLAN.		

IN THE CITY RIVERSIDE, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA.
PRELIMINARY GRADING PLAN
P.M. 36981 (PARCELS 1-6)
& P.M. 36888 (PARCELS 3-4)
 CITY OF RIVERSIDE, CA
 OCTOBER 2016



VICINITY MAP
 NOT TO SCALE
 THOMAS GUIDE PAGE: 715 GRID: A7

OWNER/APPLICANT

REX WATER, LLC
 5700 82ND HIGHWAY
 911 EAGLE CREST
 RIVERSIDE, CA 92506

ENGINEER

SDH & ASSOCIATES, INC.
 5025 CANYON CREST DRIVE 71430
 RIVERSIDE, CA 92507
 VOICE: (951) 483-3891
 FAX: (951) 708-2114

LEGAL DESCRIPTION

PARCELS 1-6 OF PM 36981
 PARCELS 3-4 OF PM 36888

SCHOOL DISTRICT

RIVERSIDE UNIFIED
 SCHOOL DISTRICT

ZONING AND LAND USE

EXISTING ZONING - BWP
 PROPOSED ZONING - BWP

UTILITY PURVEYORS

WATER.....SAND
 GAS.....SOL CALIF. GAS
 ELECTRICAL.....CITY OF RIVERSIDE/ SOL CAL EDSON
 TELEPHONE.....PACIFIC BELL
 SEWER.....CITY OF RIVERSIDE
 CABLE.....CHARTER COMMUNICATION

SURROUNDING LAND USE

NORTH.....VACANT
 EAST.....RESIDENTIAL/COMMERCIAL/VACANT
 SOUTH.....VACANT
 WEST.....COMMERCIAL

SURROUNDING ZONING

NORTH.....BWP
 EAST.....N/A - MORENO VALLEY
 SOUTH.....BWP
 WEST.....BWP

LEGEND

- T.B. - TOP OF BERM
- F.G. - FINISHED GRADE
- F.L. - FLOW LINE
- H.P. - HIGH POINT
- EXIST. - EXISTING
- E.L. - END ELEVATION
- G.B. - GRADE BROWN
- TRACT BOUNDARY
- CENTERLINE
- CURB AND GUTTER
- EXISTING CONTOUR LINE
- LOT LINE
- SLOPE

CONSTRUCTION NOTES:

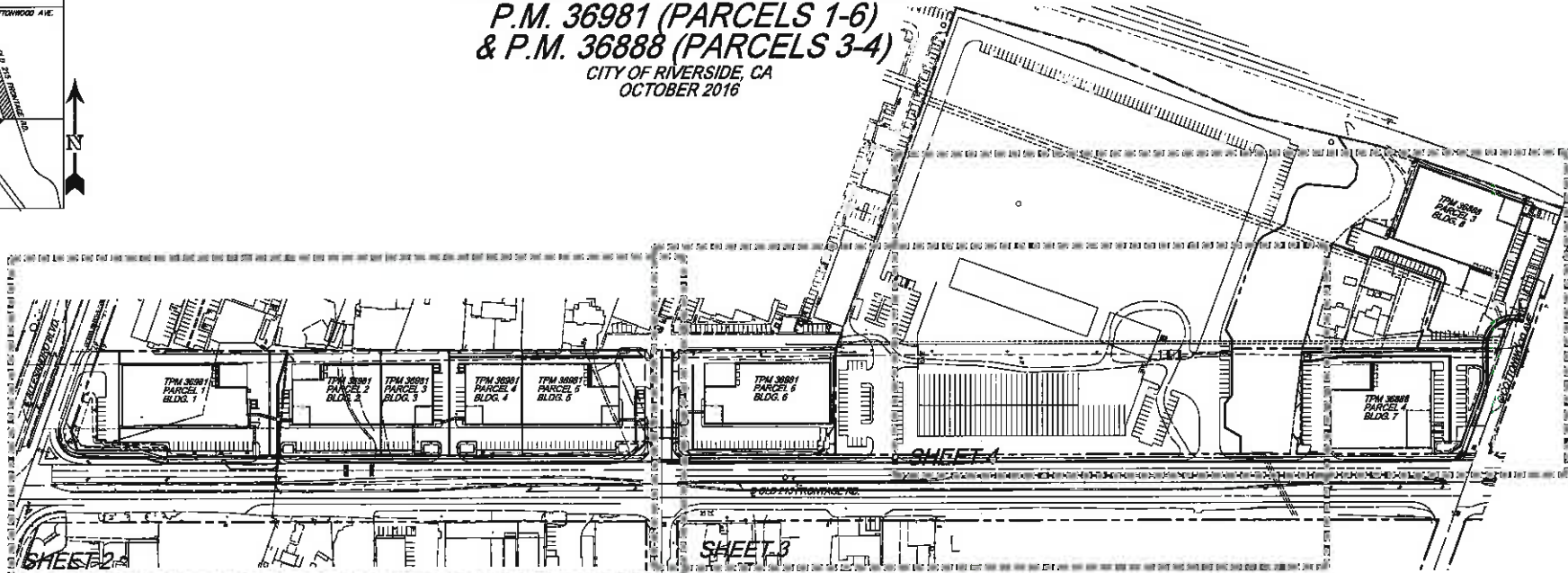
- 1) CONSTRUCT 6" CONC OVER NATIVE
- 2) CONSTRUCT 6" CURB ONLY PER DETAIL "A" HEREON
- 3) CONSTRUCT 6" CURB AND GUTTER PER DETAIL "B" HEREON
- 4) CONSTRUCT RIBBON GUTTER
- 5) CONSTRUCT TRENCH DRAIN
- 6) CONSTRUCT 4" PCC SIDEWALK

PROJECT INFO

AREA OF SITE = 684,825 SF (15.3 AC)
 LANDSCAPE AREA = 84,883 (2.2 AC)
 PARKING/SUBTOWNY AREA = 242,478 (5.6 AC)
 PARKING SPACES = 181 STD. (8'X18')
 67 TRAILER (10'X25')
 7 HANGAR (10'X25')
 TOTAL BUILDING AREA = 237,599 (5.3 AC)
 OFFICES = 5,000 SF
 WAREHOUSE = 231,899 SF

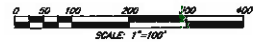
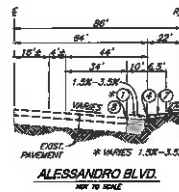
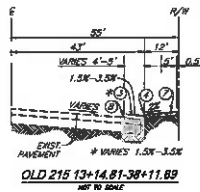
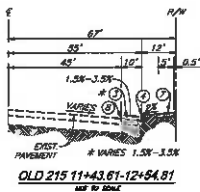
PARKING TABULATIONS

BUILDING TYPE	TOTAL PLANNED S.F.	REQUIRED PARKING
OFFICE	10,000 SF	1,250 +40 SPACES
WAREHOUSING	230,000 SF	1/1000=230 SPACES
		67 TRAILER SPACES
TOTAL BUILDING AREA	240,000 SF	175 STD. SPACES REQUIRED



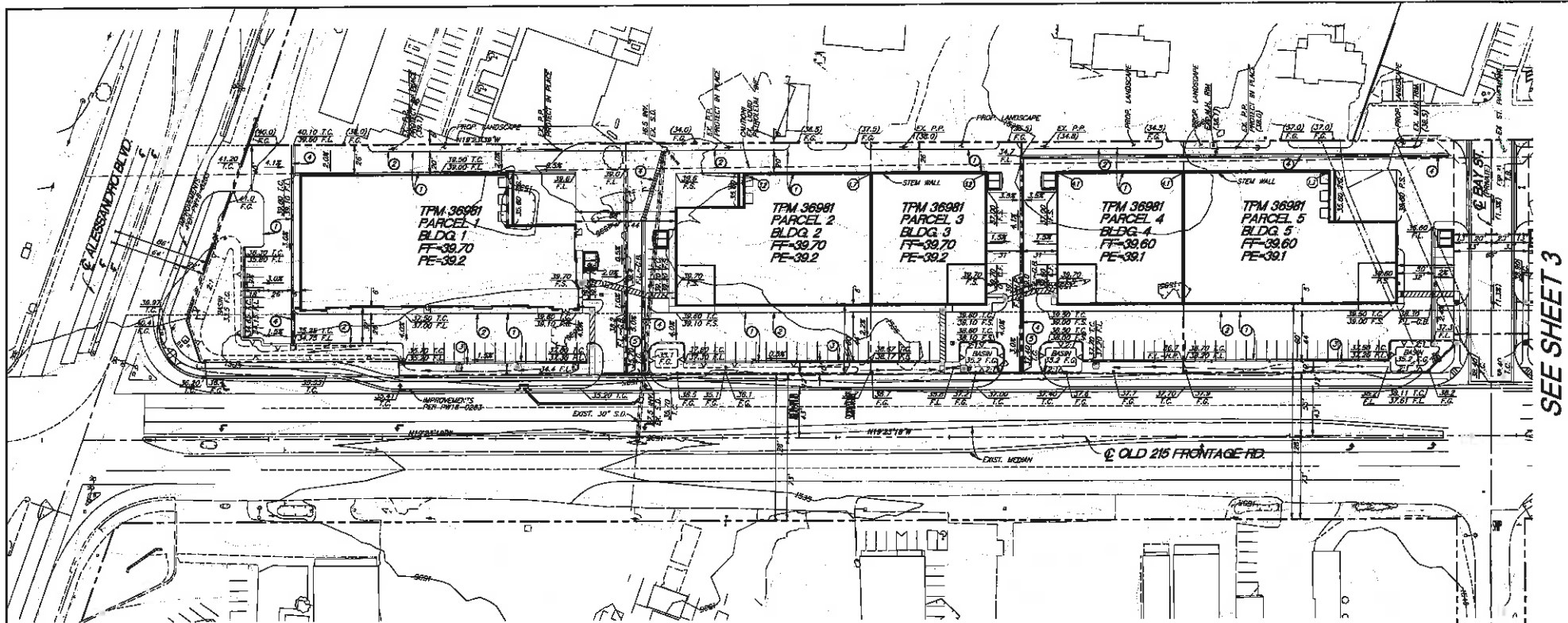
INDEX MAP

SCALE: 1"=100'



SEAL: [Professional Engineer Seal]
 PREPARED BY: SDH & ASSOCIATES, INC.
 SCALE: 1"=100'
 DATE: OCTOBER 2016

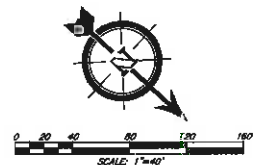
BUSINESS TAX CERTIFICATE NO. 107894 EXP. DATE 6-13-17
 CITY OF RIVERSIDE
P.M. 36981 (PARCELS 1-6)
& P.M. 36888 (PARCELS 3-4)
PRELIMINARY GRADING PLAN



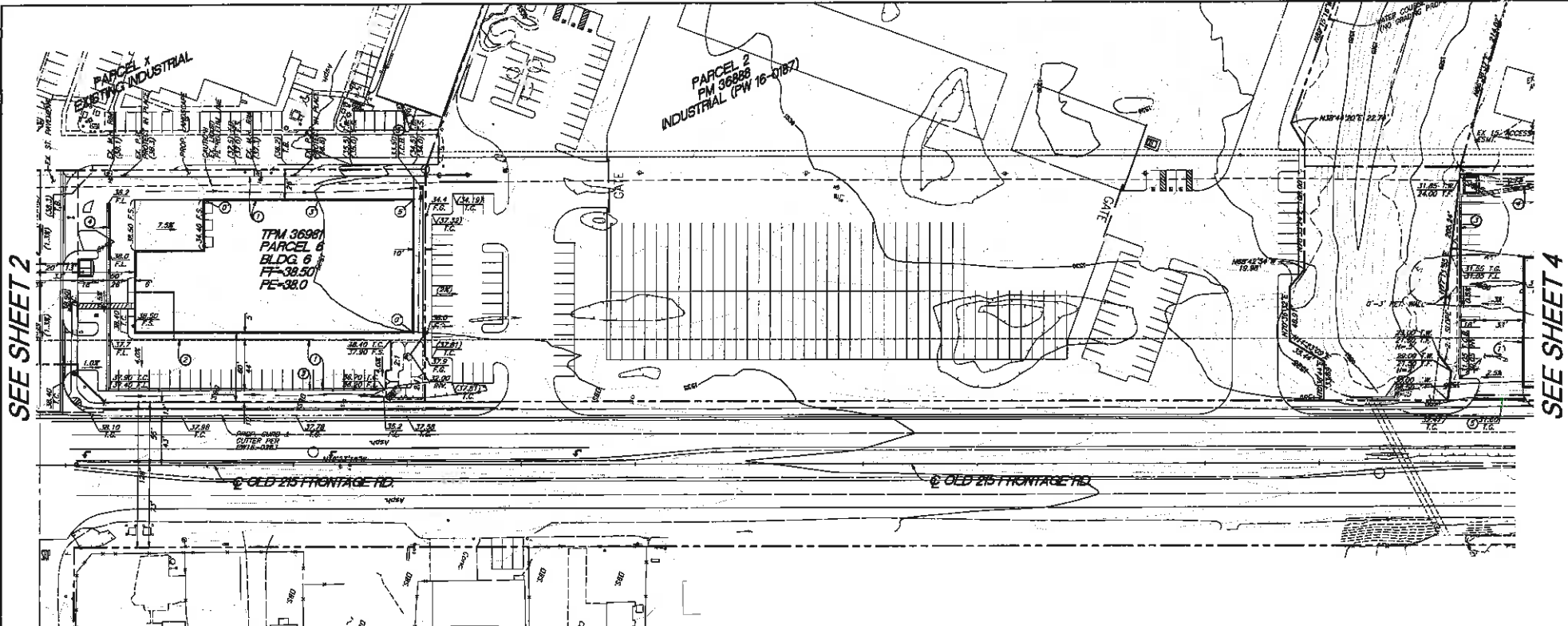
SEE SHEET 3

LEGEND

- T.B. - TOP OF BERM
 - F.G. - FINISHED GRADE
 - FL - FLOW LINE
 - H.P. - HIGH POINT
 - EXST. - EXISTING
 - P.E. - PWD ELEVATION
 - G.B. - GRADE BREAK
-
- TRACT BOUNDARY
 - CENTERLINE
 - CURB AND GUTTER
 - EXISTING CONTOUR LINE
 - 1200
 - LOT LINE
 - SLOPE



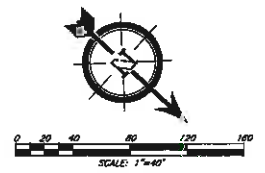
	BUSINESS TAX CERTIFICATE NO. 107994 EXP. DATE 6-13-17	CITY OF RIVERSIDE	2 OF 4 SHEETS
	PREPARED BY: SDH SON AND ASSOCIATES INC. 6285 CHAYN CREST DRIVE #1148 RIVERSIDE, CALIFORNIA 92507 TEL: (951) 885-3881 FAX: (951) 788-0214	P.M. 36981 (PARCELS 1-5) & P.M. 36888 (PARCELS 3-4) PRELIMINARY GRADING PLAN	
SCALE: 1"=40' DATE: OCTOBER 2016		BENCHMARK: FB-N3 P.K. 1482 AND CITY ENGINEER TAG IN THE CONCRETE BASE OF A STREET LIGHT POLE ALONG THE NORTHERLY CURB OF ALESSANDRO BLVD. ELEV. = 1624.840 (NAVD 88)	



SEE SHEET 2

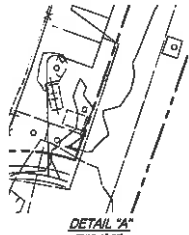
SEE SHEET 4

- LEGEND**
- T.R. - TOP OF BEAM
 - F.G. - FINISHED GRADE
 - F.L. - FLOW LINE
 - H.P. - HIGH POINT
 - EXIST. - EXISTING
 - P.E. - PAD ELEVATION
 - G.B. - GRADE BREAK
 - TRACT BOUNDARY
 - CENTERLINE
 - CURB AND GUTTER
 - EXISTING CONTOUR LINE
 - 1200
 - LOT LINE
 - SLOPE

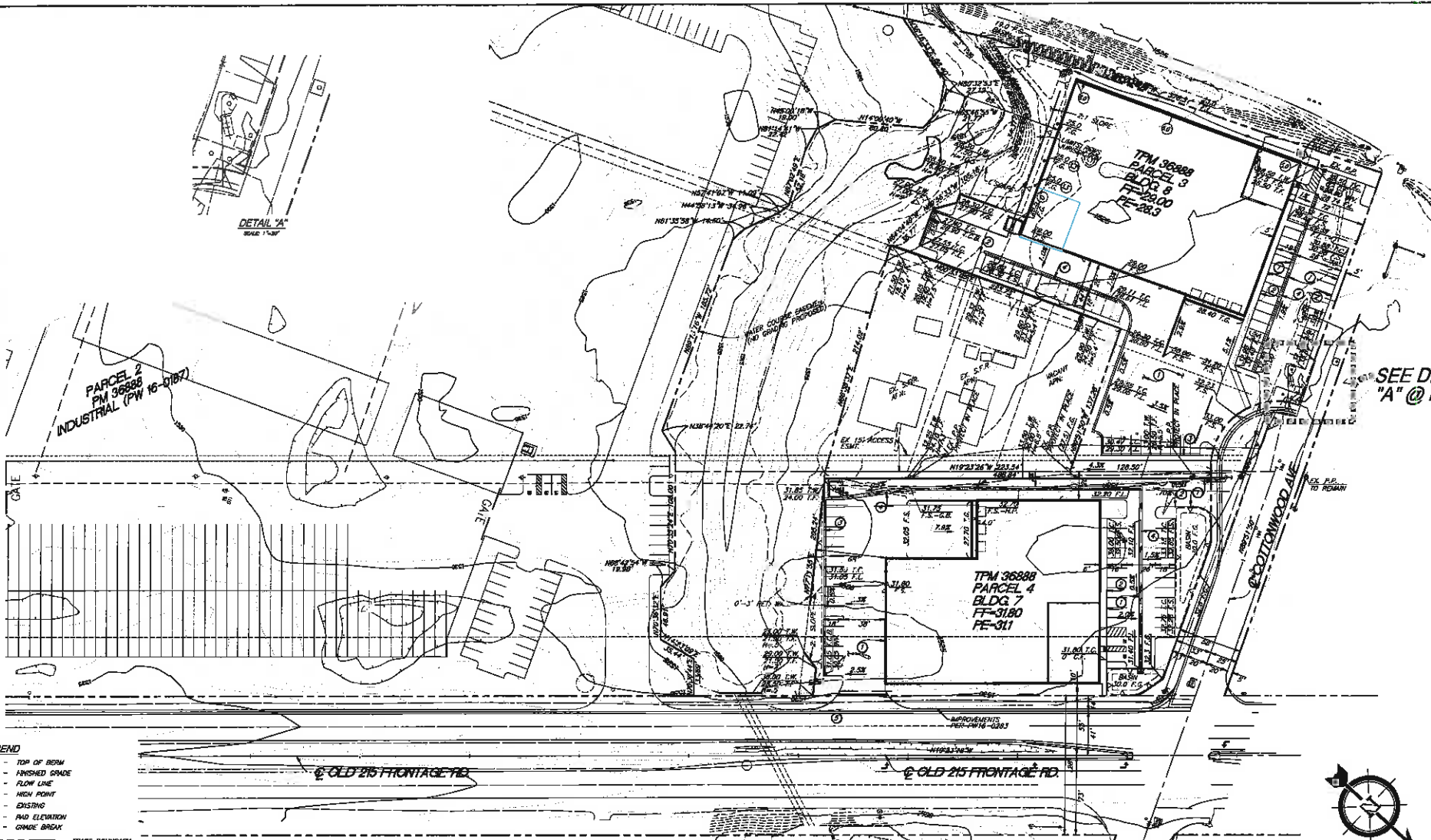


SEAL 	PREPARED BY SDW SDW AND ASSOCIATES INC. 1201 DAVENPORT STREET SUITE 11428 RIVERSIDE, CALIFORNIA 92507 TEL: (951) 945-3201 FAX: (951) 708-8014	BUSINESS TAX CERTIFICATE NO. 107994 EXP. DATE 6-13-17	CITY OF RIVERSIDE P.M. 36981 (PARCELS 1-6) & P.M. 36888 (PARCELS 3-4) PRELIMINARY GRADING PLAN	3 OF 4 SHEETS
	SCALE: 1"=40' DATE: OCTOBER 2016		BENCHMARK FB-113 P.M. NAIL AND CITY ENGINEER TAG IN THE CONCRETE BASE OF A STREET LIGHT POLE ALONG THE NORTHERLY CURB OF ALESSANDRO BLVD. ELEV. - 1824.840 (NAVD 88)	

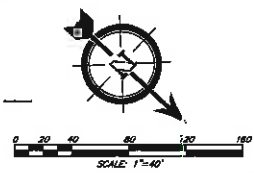
SEE SHEET 3




SEE DETAIL "A" @ LT.

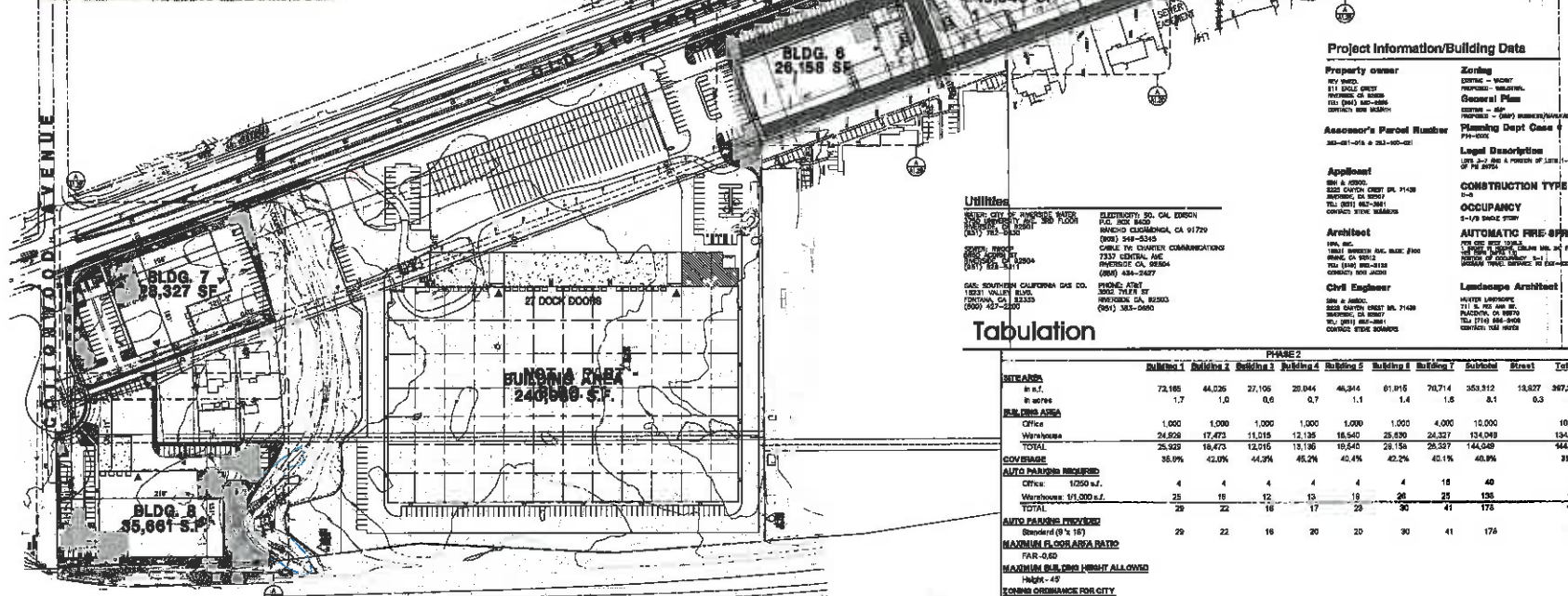


- LEGEND**
- T.B. - TOP OF BERM
 - F.G. - FINISHED GRADE
 - F.L. - FLOW LINE
 - H.P. - HIGH POINT
 - EXST. - EXISTING
 - P.E. - PAD ELEVATION
 - G.B. - GRADE BREAK
- TRACT BOUNDARY
- DOTTED LINE ---
 - CURB AND GUTTER ---
 - 1200 --- EXISTING CONTOUR LINE
 - LOT LINE ---
 - SLOPE ---



	BUSINESS TAX CERTIFICATE NO. 107904 EXP. DATE 6-13-17 PREPARED BY:	SDN SDN AND ASSOCIATES INC. 8255 CANYON CREST DRIVE, SUITE 1100 RIVERSIDE, CALIFORNIA 92507 TEL: (951) 943-3981 FAX: (951) 743-2374	CITY OF RIVERSIDE	4 OF
	SCALE: 1"=40' DATE: OCTOBER 2016	BENCHMARK: FB-113 P.A. NAME AND CITY ENGINEER TAG IN THE CONCRETE BASE OF A STREET LIGHT POLE ALONG THE NORTHERLY CURB OF ALESSANDRO BLVD. ELEV. - 1824.840 (NAVD 88)	P.M. 36881 (PARCELS 1-6) & P.M. 36888 (PARCELS 3-4) PRELIMINARY GRADING PLAN	4 SHEETS

Aerial Map



MASTER SITE PLAN
SCALE: 1" = 40'-0"

SITE PLAN KEYNOTES

- 1. TRUCK ENCLOSURE - PER CITY ORDINANCE.
- 2. ASPHALT DRIVEWAY AND PARKING SEE THIS SHEET.
- 3. CONCRETE WALLING.
- 4. CONCRETE FOUNDATION PER CITY STANDARD.
- 5. BUREAU ROAD.
- 6. FRAMED METAL MANUAL OPERATED GATES 8' W/ 4" SLOPE AND LOCK FOR FIRE DEPARTMENT ENTRANCE.
- 7. HANDICAPPED PARKING SPOTS SHALL BE:
- 8. LANDSCAPE SEE LANDSCAPE SHEET. LANDSCAPE AREAS INDICATED BY SHADDED PATTERNS.
- 9. PRE-CAST CONCRETE SHEET PILING.
- 10. DRIVE TRUCK DOCK RAMP.
- 11. HANDICAP ACCESSIBLE RAMP.

SITE PLAN GENERAL NOTES

- 1. ALL DIMENSIONS ARE TO THE FACE OF CONCRETE WALL FACE OF CONCRETE CURB OR CURB LINE UNLESS NOTED.
- 2. SEE CIVIL PLANS FOR ALL CONCRETE CURBS, CURTAINS AND WALLS.
- 3. THE DRIVE PROJECT SHALL BE PERMANENTLY SIGNAGED WITH AN AUTOMATIC OPERATION SYSTEM. PRIOR TO INSTALLATION IT AT LEAST BE DONE BEFORE BLDG. COMPLETION.
- 4. SEE CIVIL DRAWINGS FOR POINT OF CONNECTIONS TO OFF-SITE UTILITIES. CONTRACTOR SHALL VERIFY ACTUAL UTILITY LOCATIONS.
- 5. PROVIDE FLOOR DRAINAGE EVERY FLOOR LEVEL. SEE CIVIL DRAWINGS.
- 6. CONTRACTOR TO REFER TO CIVIL DRAWINGS FOR ALL HORIZONTAL CONTROL DIMENSIONS. SEE PLANS ARE FOR GUIDANCE AND STARTING LAYOUT POINTS.
- 7. SEE CIVIL DRAWINGS FOR TRUCK DRIVE CLEARANCES.
- 8. CONCRETE SCHEDULES TO BE A MINIMUM OF 4" THICK W/ TYPICAL JOINTS AT 8' O.C. EMPLOYER/CONTRACTOR JOINTS SHALL BE A MAXIMUM 12" ON ANY 1/2" TYPICAL. ALL JOINTS TO HAVE CONCRETE EXPANSION FILLER MATERIAL OF 1/4". SEE LANDSCAPE DRAWINGS FOR FINISH.
- 9. PAINT CURBS AND PROVIDE SIGNS TO AROUND OF THE LINES AS REQUIRED BY CITIES AND DEPARTMENT STANDARD 8-2-2.

- 10. CONSTRUCTION DOCUMENTS PERTAINING TO THE LANDSCAPE AND REMOVAL OF THE ENTIRE PROJECT SITE SHALL BE SUBMITTED TO THE BUILDING DEPARTMENT AND APPROVED BY PUBLIC UTILITIES DEPARTMENT PRIOR TO RESUME OF BUILDING PERMITS.
- 11. PRIOR TO FINAL CITY SUBMISSION, THE LANDSCAPE ARCHITECT SHALL SUBMIT A CERTIFICATE OF COMPLETION TO PUBLIC UTILITIES DEPARTMENT.
- 12. ALL LANDSCAPE AND IRRIGATION SYSTEMS SHALL MEET CURRENT CITY ORDINANCES AS LISTED IN GUIDANCES OR AS OBTAINED FROM PUBLIC UTILITIES DEPARTMENT.
- 13. UNDEVELOPED AREAS SHALL BE DELIMITED WITH A MINIMUM SIX INCHES (6") HIGH CURB.
- 14. APPROPRIATE CONCEPTUAL LANDSCAPE PLAN PRIOR TO GRASSING PERMITS.
- 15. PROVIDE RECESSED WHEEL SIGN AT ALL WARE HOUSE DOCKS.

SITE LEGEND

- LANDSCAPED AREA
- AC. PAVING - SEE "C" DRAWING FOR THICKNESS
- CONCRETE FINISH SEE "C" DRAWING FOR THICKNESS
- SEE THIS SHEET FOR APPROPRIATE ACCESS SHALL BE TO BE PROVIDED WITH MARKED TRUCK DRIVE AND PARKING SPOTS WITH REVERSE FIRE DOCK.
- POINT OF AERIAL LADDER ROOF ACCESS.
- MIN. OF TRUCK.
- STANDARD PARKING - STALL OF 8' X 12'
- HANDICAP PARKING - STALL OF 8' X 18'
- F.V. FULL FREIGHT VEHICLE - STALL OF 8' X 12'
- C.P. CORPUS VEHICLE - STALL OF 8' X 18'
- USE SYMBOLS
- NOTE THE APPROPRIATE LOCATION
- W- WOOD USE - SEE CIVIL
- C- CONCRETE USE - SEE CIVIL
- S- SOFT LUMBER - SEE CIVIL

Tabulation

SITE AREA	PHASE 2								Subtotal	Street	Total
	Building 1	Building 2	Building 3	Building 4	Building 5	Building 6	Building 7	Building 8			
s.f.	73,186	44,026	27,106	28,044	46,344	61,916	70,714	353,812	18,827	967,228	s.f.
In acres	1.7	1.0	0.6	0.7	1.1	1.4	1.6	8.1	0.3	84.60	
BUILDING AREA											
Office	1,000	1,000	1,000	1,000	1,000	1,000	4,000	10,000		10,000	s.f.
Warehouse	24,928	17,473	12,016	12,180	18,540	23,820	24,327	124,048		124,048	s.f.
TOTAL	25,928	18,473	13,016	13,180	19,540	24,820	25,327	144,048		144,048	s.f.
COVERAGE	38.0%	42.0%	44.3%	45.2%	40.4%	42.2%	40.1%	40.9%		38.3%	
AUTO PARKING REQUIRED											
Office: 1/250 s.f.	4	4	4	4	4	4	16	40		40	stds
Warehouse: 1/1,000 s.f.	25	18	12	13	19	26	23	126		128	stds
TOTAL	29	22	16	17	23	30	41	176		178	stds
AUTO PARKING PROVIDED											
Standard (8' x 18')	29	22	16	20	20	30	41	178		178	stds
MAXIMUM FLOOR AREA RATIO											
PAR-0.50											
MAXIMUM BUILDING HEIGHT ALLOWED											
Height - 45'											
ZONING ORDINANCE FOR CITY											
Zoning Designation - BMP											

SITE AREA	Building 8	
	s.f.	80,248
In acres	2.0	ac
BUILDING AREA		
Office	2,500	s.f.
Warehouse	2,500	s.f.
TOTAL	5,000	s.f.
COVERAGE	40.0%	
AUTO PARKING REQUIRED		
Office: 1/250 s.f.	20	stds
Warehouse: 1/1,000 s.f.	25	stds
TOTAL	45	stds
AUTO PARKING PROVIDED		
Standard (8' x 18')	61	stds
MAXIMUM FLOOR AREA RATIO		
PAR-0.50		
MAXIMUM BUILDING HEIGHT ALLOWED		
Height - 45'		
ZONING ORDINANCE FOR CITY		
Zoning Designation - BMP		
SETBACK		
Front - Building over 30' high:	45'	
Building under 30' high:	30'	
Side - 0'		
Rear - 0'		

Project Information/Building Data

Property owner: BNP INC. 111 WOOD STREET, BOSTON, MA 02114

Assessor's Parcel Number: 003-01-018 & 013-00-001

Applicant: BNP INC. 111 WOOD STREET, BOSTON, MA 02114

Architect: HPA INC. 1831 BARBOUR AVENUE, LOS ANGELES, CA 90015

Civil Engineer: HPA INC. 1831 BARBOUR AVENUE, LOS ANGELES, CA 90015

Project Information/Building Data: Zoning - B-2, General Plan - G-2, Planning Dept Case # 18-000, Legal Description - LOT 1 & 2 OF A PORTION OF L.P.M. 1-0 OF 2007, CONSTRUCTION TYPE - 1-8, OCCUPANCY - 5-1/8 SINGLE STORY, AUTOMATIC FIRE SPRINKLER, LANDSCAPE ARCHITECT - HPA INC. 1831 BARBOUR AVENUE, LOS ANGELES, CA 90015

HPA INC.
1831 Barbour Avenue, Los Angeles, CA 90015
Tel: (310) 493-1770
Fax: (310) 493-1851
Email: info@hpa.com

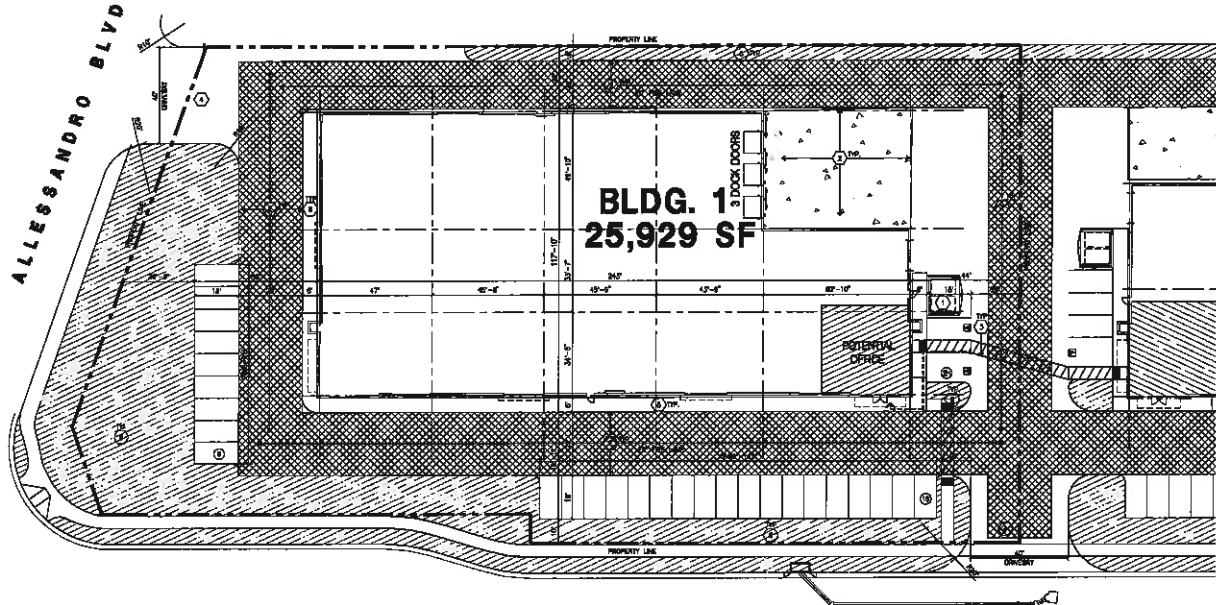
REV WHEELS BUSINESS PARK

Project: **REV WHEELS BUSINESS PARK**

File # CA

Consultants:

- CIVIL: SOH & ASSOC.
- STRUCTURAL: WARMER
- MECHANICAL: ACS
- PLUMBING: JOHNSON
- ELECTRICAL: V&L
- LANDSCAPE: HUNTER
- FIRE PROTECTION: CARSON
- SOIL ENGINEER: NORCAL



OLD 215 FRONTAGE ROAD

OVERALL SITE PLAN
TRUE PLAN NORTH

SITE PLAN KEYNOTES

1. TRASH ENCLOSURE, PER CITY REQUIREMENTS.
2. ASPHALT/CONCRETE (AC) FINISH, SEE CIVIL DRAWING.
3. CONCRETE BRUSHING.
4. UNIFORM SPACING FOR CITY SIGNAGE.
5. SEE SIGN.
6. PROVIDE METAL MANUAL OPERATED GATES BY WALK-PAD LOCK FOR FIRE DEPARTMENT STANDSTOPS. GATES TO BE OPERATED FOR THE MAIN BRIDGE ROAD, EXH. "C".
7. HANDICAPPED PARKING SHALL BE 2%.
8. LANDSCAPE, SEE LANDSCAPE DESIGN. LANDSCAPE AREAS INDICATED BY SHADDED PATTERNS.
9. PRE-CAST CONCRETE WHEEL STOP.
10. DRIVE SHAFT DOCK RAMP.
11. HANDICAP ACCESSIBLE RAMP.

SITE PLAN GENERAL NOTES

1. ALL DIMENSIONS ARE TO THE FACE OF CONCRETE SHALL, FACE OF CONCRETE CURB OR END LINE UNLESS NOTED.
2. SEE CIVIL PLANS FOR ALL CONCRETE CURBS, UTILITY AND SIGNALS.
3. THE DRAINAGE PROJECT SHALL BE PERMANENTLY UNCHANGED WITH AN AUTOMATIC INLET/OUTLET SYSTEM. PRIOR TO INSTALLATION & AT LEAST 90 DAYS BEFORE BUILDING COMPLETION.
4. SEE CIVIL DRAWINGS FOR POINT OF CONNECTION TO OFF-SITE UTILITIES. ENGINEER SHALL VERIFY ACTUAL UTILITY LOCATIONS.
5. PROVIDE POSITIVE DRAINAGE AWAY FROM BUILDING. SEE CIVIL DRAWINGS.
6. CONTRACTOR TO REFER TO CIVIL DRAWINGS FOR ALL HORIZONTAL CONTROL DIMENSIONS. SEE PLANS AND FOR DISTANCE AND TURNING POINTS.
7. SEE CIVIL DRAWINGS FOR FINISH DRIVE ELEVATIONS.
8. CONCRETE SIGNALS TO BE A MINIMUM OF 4" THICK & / OR 10" DIA. JOINTS AT 4" O.C. CONCRETE CONSTRUCTION JOINTS SHALL BE A MAXIMUM 12" IN ANY 8' BY 100' MAX. SLAB. CONCRETE JOINTS TO HAVE COMPRESSIVE DIMENSION FULLER MAXIMUM OF 1/4". SEE LANDSCAPE DRAWINGS FOR FINISH.
9. PAINT GAMES AND PROVIDE SIGNS TO IMPROVE FIRE LANE AS INDICATED BY SHADDED FIRE DEPARTMENT STANDSTOPS (S-100).
10. CONSTRUCTION DOCUMENTS PERTAINING TO THE LANDSCAPE AND REMOVAL OF THE EXISTING PROJECTS ARE TO BE SUBMITTED TO THE BUILDING DEPARTMENT AND APPROVED BY PUBLIC FACILITIES DEVELOPMENT PRIOR TO ISSUANCE OF BUILDING PERMITS.
11. PRIOR TO FINAL CITY APPROVAL, THE LANDSCAPE ARCHITECT SHALL SUBMIT A CERTIFICATE OF COMPLETION TO PUBLIC FACILITIES DEVELOPMENT.
12. ALL LANDSCAPE AND SIGNAGE REQUIREMENTS SHALL MEET CURRENT CITY REQUIREMENTS AS LISTED IN ORDINANCES OR AS OTHERWISE FROM PUBLIC FACILITIES DEVELOPMENT.
13. UNDESIGNED AREAS SHALL BE DELINEATED WITH A MINIMUM 2" DIA. (6") HIGH CURB.
14. APPROVED CONCEPTUAL LANDSCAPE PLAN PRIOR TO GRADING PERMITS.
15. PROVIDE EXCESSIVE SIGNAGE AT ALL MAIN ENTRY DOORS.

SITE LEGEND

LANDSCAPED AREA	STANDARD PARKING STALL (8' X 18')
	HANDICAP PARKING STALL (8' X 18')
CONCRETE PAVING SEE "C" DIMENSIONS FOR THICKNESS	F.E. FULL EMISSION VEHICLE STALL (8' X 18')
20' WIDE FIRE APPARATUS ACCESS ROAD TO BE IDENTIFIED WITH PAINTED RED CURBS AND MARKED FIRE LANE-NO PARKING FOR CITY OF RIVERSIDE FIRE DEPT.	D.V. DISABLED VEHICLE STALL (8' X 18')
	SIGN LOCATION - SEE CIVIL

Tabulation

PHASES	PHASES										
	Building 1	Building 2	Building 3	Building 4	Building 5	Building 6	Building 7	Subtotal	Street	Total	
SITE AREA											
In s.f.	69,248	1,000	1,000	1,000	1,000	1,000	4,000	10,000		10,000	
In acres	2.0										
BUILDING AREA											
Office	2,500									2,500	
Warehouse	2,500									2,500	
TOTAL	5,000									5,000	
COVERAGE											
Office: 1/250 s.f.	20									20	
Warehouse: 1/1,000 s.f.	31									31	
TOTAL	51									51	
AUTO PARKING PROVIDED											
Standard (8' x 18')	61									61	
MAXIMUM FLOOR AREA RATIO											
FAR-0.80											
MAXIMUM BUILDING HEIGHT ALLOWED											
Height - 40'											
ZONING ORDINANCE FOR CITY											
Zoning Designation - BMAP											
SETBACKS											
Front - Building over: 30' High: 40'											
Building under: 30' High: 20'											
Side - 0'											
Rear - 0'											

Aerial Map



HPA
ARCHITECTURE

100 W. 112nd Street, Suite 100 • Los Angeles, CA 90001
Tel: 310-441-1111
Fax: 310-441-1112
www.hpaarch.com

REV
WHEELS

10000
K-0000

Project

REV WHEELS BUSINESS PARK

Phase 1A

Consultants

ARCHITECT: HPA
MECHANICAL: HPA
PLUMBING: HPA
ELECTRICAL: HPA
LANDSCAPE: HPA
STRUCTURE: HPA
ELEVATION: HPA

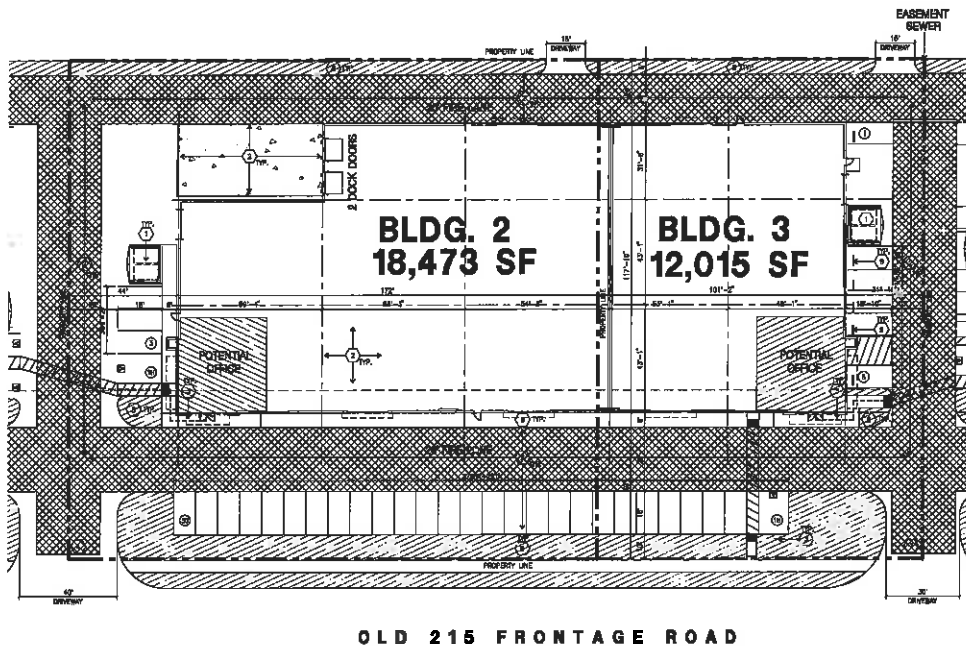
Project Number: 10118

Drawn by: CC

Date: 08/18/10

Scale:

A1.1A



SITE PLAN KEYNOTES

1. TRASH ENCLOSURE PER CITY ORDINANCE
2. ASPHALT CONCRETE (AC) PAVING, SEE CIVIL DRAW.
3. CONCRETE BALUNY.
4. DIMENSION APPROX PER CITY STANDARDS.
5. BONE PAVES.
6. PROVIDE METAL, SIGNAL OPERATED GATES BY 1003-PAC LOCK PER FIRE DEPARTMENT ORDINANCE. GATE TO BE OPERATED FOR 100 MPH WIND LOAD, EXP. "C".
7. HANDICAPPED PARKING STALL SIGN.
8. LANDSCAPE SEE LANDSCAPE DIMS. LANDSCAPE AREAS INDICATED BY SHADED PATTERN.
9. PRE-CAST CONCRETE RIBBED TOP.
10. DRIVE IN/OUT DOOR SWAP.
11. HANDICAP ACCESSIBLE RAMP.

SITE PLAN GENERAL NOTES

1. ALL DIMENSIONS ARE TO THE FACE OF CONCRETE WALL, FACE OF CONCRETE CURB OR END LINE WALL.
2. SEE CIVIL PLANS FOR ALL CONCRETE CURBS, GUTTERS AND WALLS.
3. THE ENTIRE PROJECT SHALL BE PERMANENTLY MAINTAINED WITH AN AUTOMATIC EMERGENCY SERVICE. PRIOR TO INSTALLATION & AT LEAST 90 DAYS BEFORE BIDD. COMPLETION.
4. SEE CIVIL DRAWINGS FOR POINT OF CONNECTIONS TO OFF-SITE UTILITIES. CONTRACTOR SHALL VERIFY ACTUAL UTILITY LOCATIONS.
5. PROVIDE POTTING SHAWNSHAWT PANSI PLANTS WITH CIVIL DRAWINGS. CONTRACTOR TO REFER TO CIVIL DRAWINGS FOR ALL HORIZONTAL CONTROL DIMENSIONS. SITE PLANS ARE FOR GUIDANCE AND SCHEDULING LAYOUT PURPOSES.
6. SEE CIVIL DRAWINGS FOR FRESH GRADE ELEVATIONS.
7. CONCRETE REINFORCING TO BE A MINIMUM OF #4 TIECS BY TIEDED JOINTS AT 6" O.C. EXPANDED/CONSTRUCTION JOINTS SHALL BE A MINIMUM 12" SA. BY 16" TIECS SA. REINFORCING JOINTS TO HAVE COMPRESSIVE DEFLECTION FILLER MATERIAL OF 1/4". SEE LANDSCAPE DIMENSIONS FOR FINISH.
8. PAINT CURBS AND PAVEMENT EDGES TO MATCH OF FINE LINES AS REQUIRED BY DIVISION FIRE DEPARTMENT STANDARD 3-001.
9. CONSTRUCTION DOCUMENTS PERTAINING TO THE LANDSCAPE AND IRRIGATION OF THE LIVING PROJECT SITE SHALL BE SUBMITTED TO THE BUILDING DEPARTMENT AND APPROVED BY PUBLIC FACILITIES DEVELOPMENT PRIOR TO ISSUANCE OF BUILDING PERMITS.
11. PRIOR TO FINAL CITY INSPECTION, THE LANDSCAPE ARCHITECT SHALL SUBMIT A CERTIFICATE OF COMPLETION TO PUBLIC FACILITIES DEVELOPMENT.
12. ALL LANDSCAPE AND IRRIGATION DIMENSIONS SHALL MATCH EXISTING CITY STANDARDS AS LISTED IN ORDINANCES OR AS OBTAINED FROM PUBLIC FACILITIES DEVELOPMENT.
13. LANDSCAPED AREAS SHALL BE DELINEATED WITH A MINIMUM 1/2" HIGH CURB.
14. APPROVED CONCEPTUAL LANDSCAPE PLAN PRIOR TO DRIVING PERMIT.
15. PROVIDE RECESSED WEDGE BOX AT ALL MAIN ENTRY DOORS.

SITE LEGEND

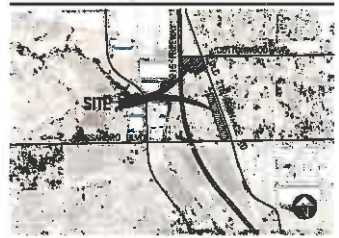
LANDSCAPED AREA	STANDARD PARKING STALL (9' X 18')
AC PAVING - SEE "C" DIMS. FOR THICKNESS	HANDICAP PARKING STALL (9' X 18')
CONCRETE PAVING SEE "C" DIMS. FOR THICKNESS	FUEL EFFICIENT VEHICLE STALL (9' X 18')
FIRE FIRE APPARATUS ACCESS ROAD. ROAD TO BE IDENTIFIED WITH PAINTED RED CURBS AND MARKED "FIRE LANE-NO PARKING" PER CITY OF INVERNESS FIRE DEPT.	CARPOOL VEHICLE STALL (9' X 18')
POINT OF AERIAL LADDER ROOF ACCESS.	LIGHT SHADED
PATH OF TRAVEL. PROVIDE A SIGN AT EVERY JUNCTION OF ACCESSIBLE PATH DISPLAYING THE APPROPRIATE SYMBOLS OF ACCESSIBILITY. SIGNS SHALL INDICATE THE DIRECTION TO ACCESSIBLE FACILITY ENTRANCE AND CORRIDOR WITH 11" HIGHS THROUGH 11" HIGHS.	WIDE LANE - SEE CIVIL
	ONE LANE - SEE CIVIL
	WIDE LANE - SEE CIVIL

TRUE PLAN NORTH NORTH

Tabulation

PHASE 3	Building #	PHASE 2										Street	Total
		Subarea 1	Subarea 2	Subarea 3	Subarea 4	Subarea 5	Subarea 6	Subarea 7	Subarea 8	Subarea 9	Subarea 10		
SITE AREA													
in s.f.	88,248												297,239
in acres	2.0												8.4
BUILDING AREA													
Office	2,800											10,000	
Mezzanine	2,500											134,015	
Warehouse	30,891											144,048	
TOTAL	36,191											188,063	
COVERAGE	40.1%												
AUTO PARKING REQUIRED													
Office: 1/250 s.f.	20											136	
Warehouse: 1/1,000 s.f.	31											178	
TOTAL	51											314	
AUTO PARKING PROVIDED													
Standard (9' x 18')	81											178	
MAXIMUM FLOOR AREA RATIO													
FAR-0.50												0.50	
MAXIMUM BUILDING HEIGHT ALLOWED													
Height - 45'												45'	
ZONING ORDINANCE FOR CITY													
Zoning Designation - BMP												BMP	
SETBACK													
Front - Building over 30' high: 40'												40'	
Side - 4'												4'	
Rear - 0'												0'	

Aerial Map



HPA
HILLTOP PLANNING ASSOCIATES, INC.
104, Inc.
18651 Sanderson Avenue, - Ste. #100
Inverness, FL 33412
Tel: 941-863-1770
Fax: 941-863-0801
email: hpa@hpa1.com

REV
WHEELS
BUSINESS PARK

Project:
REV WHEELS BUSINESS PARK

Consultants:
CIVIL: SDA B ASSOC.
STRUCTURAL: KRUMHOLTZ
MECHANICAL: ACS
ELECTRICAL: JOHNSON
LANDSCAPE: HUNTER
IREPRETOR: CAPRON
SEASSEMBLER: MORCAL

Title: Overall site plan

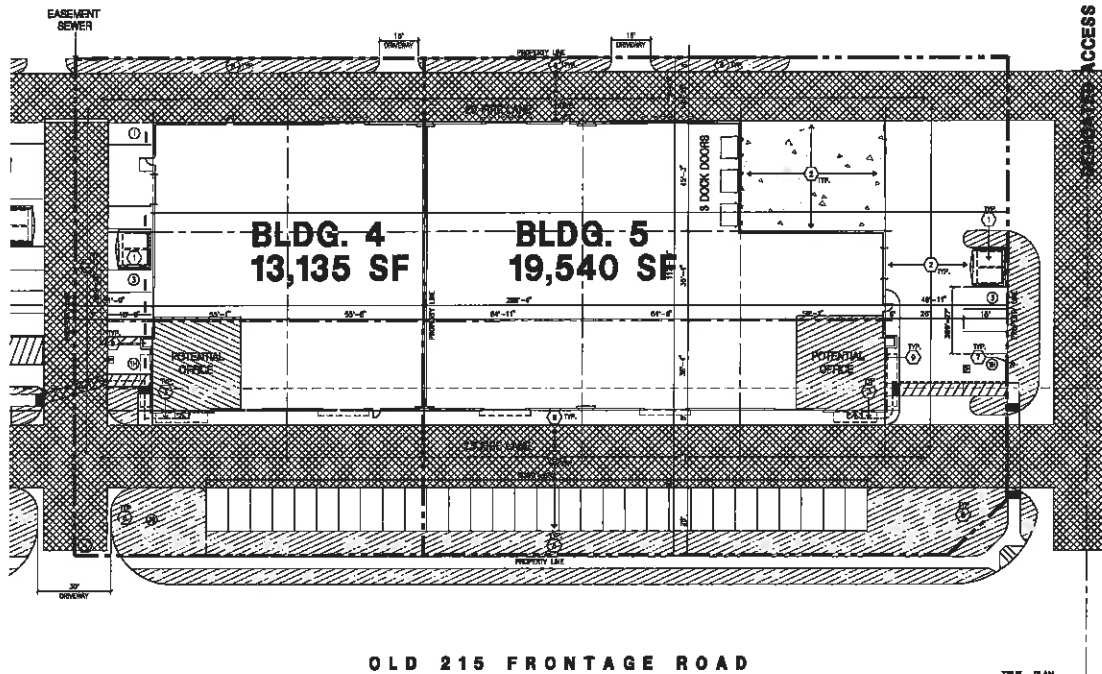
Project Number: 1818

Drawn by: CC

Date: 06/26/19

Revision:

Sheet: **A1.1B**



TRUCK PLAN NORTH
OVERALL SITE PLAN
 SHEET 1 OF 3

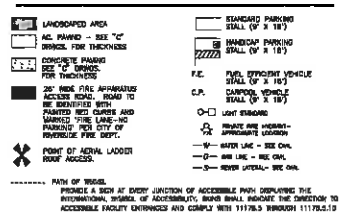
SITE PLAN KEYNOTES

1. TRUCK ENCLOSURE FOR CITY STANDING.
2. ASPHALT CONCRETE (AC) PAVING, SEE CIVIL DRAWING.
3. CONCRETE WALKWAY.
4. CEMENT ASPHALT PER CITY STANDING.
5. BUMP TRUCK.
6. PROVIDE METAL, MANUAL OPERATED GATES 8' / 4001-P40 LEAF FOR THE SEWER/UTILITY ENCLOSURE. GATE TO BE OPERATED FOR THE 80% WIND LOAD, EXP. "C".
7. HANDICAPPED PARKING STALL SIGN.
8. LANDSCAPE, SEE LANDSCAPE DESIGN. LANDSCAPE AREAS INDICATED BY SHADDED PATTERNS.
9. PRE-CAST CONCRETE WHEEL STOP.
10. DRIVE THRU DOCK RAMP.
11. HANDICAP ACCESSIBLE RAMP.

SITE PLAN GENERAL NOTES

1. ALL DIMENSIONS ARE TO THE FACE OF CONCRETE WALL, FACE OF CONCRETE CURB OR OLD LINE UTIL. LOC.
2. SEE CIVIL PLANS FOR ALL CONCRETE CURBS, BATTERS AND SLOPES.
3. THE ENTIRE PROJECT SHALL BE PERMANENTLY MAINTAINED WITH AN AUTOMATIC MAINTENANCE SCHEDULE. PRIOR TO INSTALLATION & AT LEAST 90 DAYS BEFORE BLDG. COMPLETION.
4. SEE CIVIL DRAWINGS FOR POINT OF CONNECTIONS TO OFF-SITE UTILITIES. CONTRACTOR SHALL VERIFY ACTUAL UTILITY LOCATIONS.
5. PROVIDE POSITIVE DRAINAGE AWAY FROM BLDG. SEE CIVIL DRAWINGS.
6. CONTRACTOR TO REFER TO CIVIL DRAWINGS FOR ALL HORIZONTAL CONTROL DIMENSIONS. SITE PLANS ARE FOR CLARIFICATION AND LAYOUT POINTS.
7. SEE CIVIL DRAWINGS FOR FINISH CONCRETE DIMENSIONS.
8. CONCRETE SHALL BE TO BE A MINIMUM OF 4" THICK 1/2" TYPED JOINTS AT 4' INTERVALS. CONCRETE JOINTS SHALL BE A MINIMUM 1/2" DIA. SEE 1/2" DIA. HOLE. CONCRETE JOINTS TO HAVE COMPRESSIVE EXPANSION PATCH MATERIAL OF 1/2". SEE LANDSCAPE DRAWINGS FOR FINISH.
9. PAINT COATERS AND FINISH SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE FIRE DEPARTMENT STANDARD S-105.
10. CONSTRUCTION DOCUMENTS PERTAINING TO THE LANDSCAPE AND DIMENSIONS OF THE ENTIRE PROJECT SITE SHALL BE SUBMITTED TO THE BUILDING DEPARTMENT AND APPROVED BY PUBLIC FACILITIES DEVELOPMENT PRIOR TO RELEASE OF BUILDING PERMITS.
11. PRIOR TO FINAL CITY APPROVAL, THE LANDSCAPE ARCHITECT SHALL SUBMIT A CERTIFICATE OF COMPLETION TO PUBLIC FACILITIES DEVELOPMENT.
12. ALL LANDSCAPE AND FINISHING DETAILS SHALL MEET CURRENT CITY STANDARDS AS LISTED IN ORDINANCES OR AS OBTAINED FROM PUBLIC FACILITIES DEVELOPMENT.
13. LANDSCAPED AREAS SHALL BE DELINEATED WITH A MINIMUM SEE BINDER (S) PER CITY.
14. APPROVED CONCEPTUAL LANDSCAPE PLAN FROM THE BUILDING DEPARTMENT.
15. PROVIDE RECESSED SIGN BOX AT ALL SIGN ENTRY DOORS.

SITE LEGEND



HPA
 CONSULTANTS

Inc., P.C.
 18811 bardonia avenue - ste. #100
 irvine, ca 92612
 tel: 949-853-1770
 fax: 949-853-6851
 email: hpa@hpapc.com

REV
 WHEELS
 BUSINESS PARK

XXXXX
 XXXXX
 XXXXX

Project:
REV WHEELS BUSINESS PARK

Rev 066, CA

Consultants:

CIVIL STRUCTURAL MECHANICAL PLUMBING ELECTRICAL LANDSCAPE FIRE PROTECTION
 SOH & ASSOC. KRANER, ACS JOHNSON VAIL HANLON GARDNER NORCAL

Tabulation

PHASE 3		PHASE 2									
Buildings 9		Building 1	Building 2	Building 3	Building 4	Building 5	Building 6	Building 7	Subtotal	Subtotal	Total
SITE AREA	In s.f.	72,166	44,028	27,106	29,044	48,344	61,816	70,714	352,812	12,827	347,238 s.f.
	In acres	1.7	1.0	0.8	0.7	1.1	1.4	1.6	8.1	0.3	8.8 ac
BUILDING AREA											
Office	2,500 s.f.	1,000	1,000	1,000	1,000	1,000	1,000	4,000	10,000		10,000 s.f.
Mechanical	2,500 s.f.	24,929	17,473	11,015	12,135	16,540	25,692	24,327	134,049		134,049 s.f.
Warehouse	30,691 s.f.	25,829	18,473	12,015	13,135	19,540	26,153	28,327	144,449		144,449 s.f.
TOTAL	35,691 s.f.										35,691 s.f.
COVERAGE		36.6%	42.0%	44.3%	45.2%	40.4%	42.3%	40.1%	40.8%		39.2%
AUTO PARKING PROVIDED											
Office	1,650 s.f.	4	4	4	4	4	4	18	40		40 stalls
Warehouse	1/1,000 s.f.	26	16	12	13	18	26	26	138		138 stalls
TOTAL		29	22	16	17	23	30	41	178		178 stalls
AUTO PARKING PROVIDED											
Standard (9' x 18')											61 stalls
MAXIMUM FLOOR AREA RATIO											
FAR-0.50		20	22	16	30	30	30	41	178		178 stalls
MAXIMUM BUILDING HEIGHT ALLOWED											
Height - 45'											
ZONING ORDINANCE FOR CITY											
Zoning Designation - BMP											
SETBACK											
Front - Building over 30' High: 40'											
Side - 0'											
Rear - 0'											

Aerial Map



Title: Overall site plan

Project Number: 16125

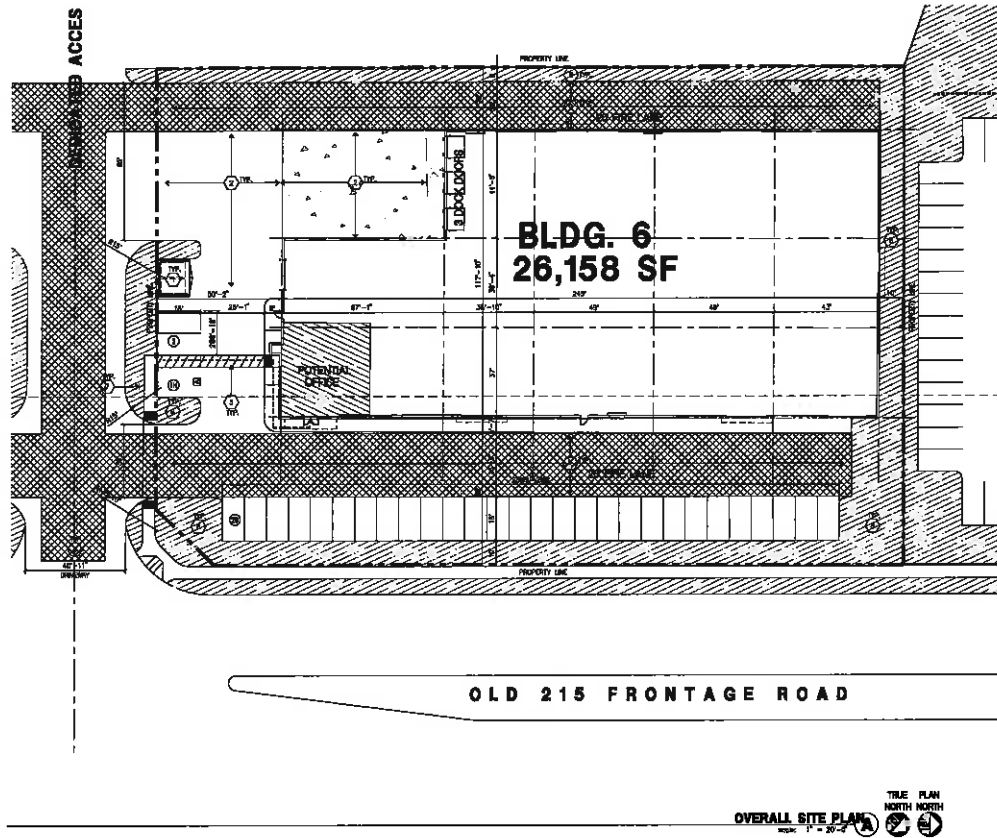
Drawn by: CC

Date: 08/05/2016

Revision:

Sheet

A1.1C



SITE PLAN KEYNOTES

1. IRISH DRAINAGE, FOR CITY STANDARDS.
2. ASPHALT CONCRETE (AC) PAVING, SEE CIVIL DRAWING.
3. CONCRETE WALKWAY.
4. DRIVEWAY APPROVED PER CITY STANDARDS.
5. IRISH BRICK.
6. PROVIDE METAL MANHOLE EXTENDED OUTLET W/ 1000-PAD LOAD FOR THE DEPARTMENT OF ENVIRONMENT.
7. MANHOLE TO BE DESIGNED FOR 100 BPS WIND LOAD, EXP. 10".
8. HANDICAPPED PARKING STALL SIGN.
9. LANDSCAPE, SEE LANDSCAPE DESIGN, LANDSCAPE AREAS INDICATED BY SHADING PATTERNS.
10. PRE-CAST CONCRETE WHEEL STOP.
11. DRIVE THRU DOCK RAMP.
12. HANDICAP ACCESSIBLE RAMP.

SITE PLAN GENERAL NOTES

1. ALL DIMENSIONS ARE TO THE FACE OF CONCRETE OR FACE OF CONCRETE CURB ON GRID LINE LINES.
2. SEE CIVIL PLANS FOR ALL CONCRETE CURBS, CURTAINS AND BENCHES.
3. THE ENTIRE PROJECT SHALL BE PERMANENTLY MAINTAINED WITH AN AUTOMATIC WINDSHIELD WIPER.
4. PRIOR TO INSTALLATION IS AT LEAST 90 DAYS BEFORE BLDG. COMPLETION.
5. SEE CIVIL DRAWINGS FOR POINT OF CONNECTION TO OFF-SITE UTILITIES. CONTRACTOR SHALL VERIFY UTILITY LOCATIONS.
6. PROVIDE POSITIVE DRAINAGE AWAY FROM BLDG. SEE CIVIL DRAWINGS.
7. CONTRACTOR TO REFER TO CIVIL DRAWINGS FOR ALL HORIZONTAL CONTROL DIMENSIONS. SEE PLANS FOR SLOPING AND STARTING POINTS.
8. SEE CIVIL DRAWINGS FOR FINISH GRADE ELEVATIONS.
9. CONCRETE BENCHES TO BE A MINIMUM OF 4" THICK W/ TOLDED JOINTS AT 6" O.C. EXPANSION/CONTRACTION JOINTS SHALL BE A MINIMUM 1/2" DIA. BY 1/2" DIA. SLOPE EXPANSION JOINTS TO HAVE COMPRESSIVE DEPRESSION PATTERN MINIMUM OF 1/4". SEE LANDSCAPE DRAWINGS FOR FINISH.
10. PAINT CURBS AND PROVIDE SIGN TO INDICATE FIRE LINES AS INDICATED BY SHADING ARE COMPENSATED STANDARD 8-001.
11. CONSTRUCTION DOCUMENTS PERTAINING TO THE LANDSCAPE AND IRRIGATION OF THE ENTIRE PROJECT SHALL BE SUBMITTED TO THE BUILDING DEPARTMENT AND APPROVED BY PUBLIC FACILITIES DEVELOPMENT PRIOR TO ISSUANCE OF BUILDING PERMITS.
12. PRIOR TO FINAL CITY APPROVAL THE LANDSCAPE ARCHITECT SHALL SUBMIT A CERTIFICATE OF COMPLETION TO PUBLIC FACILITIES DEVELOPMENT.
13. ALL LANDSCAPE AND IRRIGATION DESIGN SHALL MEET CURRENT CITY STANDARDS AS LISTED IN SPECIFICATIONS OR AS OBTAINED FROM PUBLIC FACILITIES DEVELOPMENT.
14. LANDSCAPED AREAS SHALL BE FULFILLED WITH A MINIMUM SIX INCHES (6") HIGH CURB.
15. APPROVED CONCEPTUAL LANDSCAPE PLAN PRIOR TO ISSUANCE PERMIT.
16. PROVIDE ACCESSIBLE WALK WAY AT ALL WALK ENTRY POINTS.

SITE LEGEND

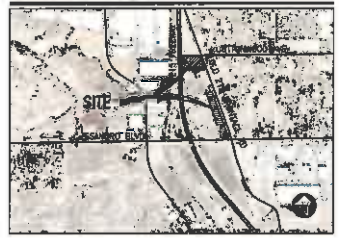
LANDSCAPED AREA	STANDARD PARKING STALL (8' X 18')
AC DRIVE - SEE "P" DRIVE FOR THICKNESS	HANDICAP PARKING STALL (8' X 18')
CONCRETE PAVING SEE "P" DRIVE FOR THICKNESS	FULL SPOKE VEHICLE STALL (8' X 18')
20' SIDE FIRE APPARATUS ACCESS ROAD TO BE DIMENSIONED WITH FINISHED SIDE CURBS AND PARKING PER CITY OF RICHMOND PER DEPT.	LIGHT SHADED
POINT OF ARRIVAL, LOADER ROOF ACCESS.	MOTOR LANE - SEE CIVIL
30' SIDE FIRE APPARATUS ACCESS ROAD TO BE DIMENSIONED WITH FINISHED SIDE CURBS AND PARKING PER CITY OF RICHMOND PER DEPT.	30' SIDE FIRE APPARATUS ACCESS ROAD TO BE DIMENSIONED WITH FINISHED SIDE CURBS AND PARKING PER CITY OF RICHMOND PER DEPT.
MOTOR LANE - SEE CIVIL	30' SIDE FIRE APPARATUS ACCESS ROAD TO BE DIMENSIONED WITH FINISHED SIDE CURBS AND PARKING PER CITY OF RICHMOND PER DEPT.
30' SIDE FIRE APPARATUS ACCESS ROAD TO BE DIMENSIONED WITH FINISHED SIDE CURBS AND PARKING PER CITY OF RICHMOND PER DEPT.	30' SIDE FIRE APPARATUS ACCESS ROAD TO BE DIMENSIONED WITH FINISHED SIDE CURBS AND PARKING PER CITY OF RICHMOND PER DEPT.

NOTE OF TRAVEL: PROVIDE A SIGN AT EVERY JUNCTION OF ACCESSIBLE PATH DISPLAYING THE INTERNATIONAL SYMBOL OF ACCESSIBILITY. SIGN SHALL INDICATE THE DIRECTION TO ACCESSIBLE ENTRY ENTRANCES AND COMPLY WITH 111783 THROUGH 111783.10

Tabulation

PHASE 1	BUILDING #	PHASE 2									
		Sublot 1	Sublot 2	Sublot 3	Sublot 4	Sublot 5	Sublot 6	Sublot 7	Subtotal	Street	Total
SITE AREA											
In a.f.	89,248 s.f.	In a.f. 72,165 44,026 27,100 20,044 46,344 61,915 70,714 253,312 13,927 247,239 s.f.									
In acres	2.0 ac	In acres 1.7 1.0 0.8 0.7 1.1 1.4 1.6 1.8 0.3 8.4 ac									
BUILDING AREA											
Office	2,800 s.f.	1,000 1,200 1,000 1,000 1,000 1,000 4,000 10,000 10,000 s.f.									
Mezzanine	2,500 s.f.	24,859 17,473 11,015 12,155 16,640 25,690 24,327 134,048 134,048 s.f.									
Warehouse	30,691 s.f.	25,929 16,473 12,016 13,136 16,540 28,158 25,327 144,049 144,049 s.f.									
TOTAL	35,991 s.f.	35,991 s.f.									
COVERAGE	40.0%	40.0%									
AUTO PARKING REQUIRED											
Office: 1/250 s.f.	20 stalls	Office: 1/250 s.f. 4 4 4 4 4 4 4 16 40 40 stalls									
Warehouse: 1/1,000 s.f.	31 stalls	Warehouse: 1/1,000 s.f. 25 16 12 13 18 26 25 138 158 stalls									
TOTAL	51 stalls	51 stalls									
AUTO PARKING PROVIDED											
Standard (8' x 18')	81 stalls	Standard (8' x 18') 28 22 18 20 20 30 41 179 179 stalls									
MAXIMUM FLOOR AREA RATIO											
FAR-040		MAXIMUM BUILDING HEIGHT ALLOWED									
MAXIMUM BUILDING HEIGHT ALLOWED		Height - 40'									
ZONING ORDINANCE FOR CITY		Zoning Ordinance - BWP									
SETBACK											
Front - Building over 30' High: 40'											
Side - 0'											
Rear - 0'											

Aerial Map



111783 THROUGH 111783.10
 111783
 111783
 111783
 111783

3000X
 3000X

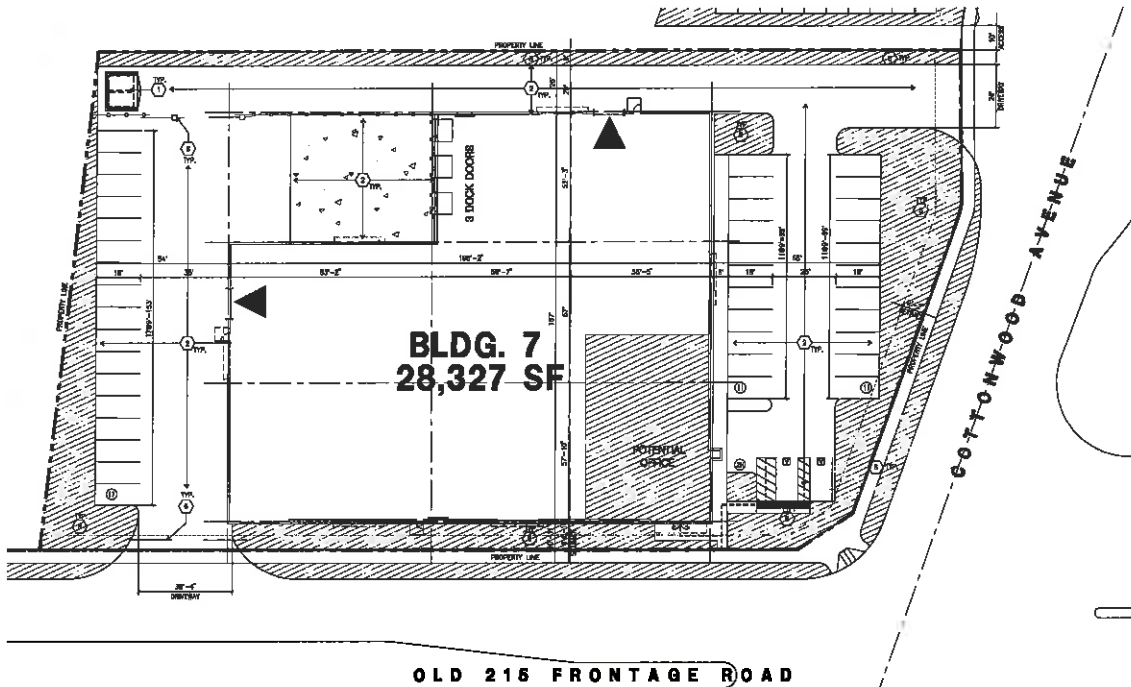
REV WHEELS BUSINESS PARK

CONTRACTORS

CITY OF RICHMOND
 CIVIL ENGINEERING
 PLANNING
 ELECTRICAL
 LANDSCAPE ARCHITECTURE
 MECHANICAL
 STRUCTURAL
 TRAFFIC ENGINEERING

CITY OF RICHMOND
 CIVIL ENGINEERING
 PLANNING
 ELECTRICAL
 LANDSCAPE ARCHITECTURE
 MECHANICAL
 STRUCTURAL
 TRAFFIC ENGINEERING

A1 ID



OVERALL SITE PLAN
Scale: 1" = 20'-0"

SITE PLAN KEYNOTES

1. FINISH FLOORING, PER CITY CODEBOOK.
2. ASPHALT CONCRETE (AC) PAVING, SEE CIVIL DRAW.
3. CONCRETE BRINKWAY.
4. DRAINAGE APPROX PER CITY STANDARD.
5. BULK PAVES.
6. PROVIDE METAL, MANHOLE OPERATED GATES BY INCHES-FRAG LOCK PER FIRE DEPARTMENT REQUIREMENTS. GATES TO BE OPERATED FOR 100 MPH WIND LOADS, ESH, "C".
7. LANDSCAPED PARKING STALLS.
8. LANDSCAPE SEE LANDSCAPE DESIGN. LANDSCAPE AREAS INDICATED BY SHADDED PATTERN.
9. FIRE-CRAT CONC. WHEEL STOP.
10. DRIVE PAVES UNDER RAMP.
11. HANDICAP ACCESSIBLE RAMP.

SITE PLAN GENERAL NOTES

1. ALL DIMENSIONS ARE TO THE FACE OF CONCRETE WALL, FACE OF CONCRETE CURB OR ONE LINE CURB.
2. SEE CIVIL PLANS FOR ALL CONCRETE CURBS, WALLS AND RAMP.
3. THE ENTIRE PROJECT SHALL BE PERMANENTLY MARKED WITH AN AUTOMATIC WARNING SYSTEM.
4. PRIOR TO INSTALLATION & AT LEAST 30 DAYS BEFORE BLDG. COMPLETION, SEE CIVIL DRAWINGS FOR POINT OF CONNECTION TO OFF-SITE UTILITIES. CONTRACTOR SHALL VERIFY ACTUAL UTILITY LOCATIONS.
5. PROVIDE POSITIVE DRAINAGE AWAY FROM BLDG. SEE CIVIL DRAWINGS.
6. CONTRIBUTOR TO REFER TO CIVIL DRAWINGS FOR ALL HORIZONTAL, VERTICAL, DRAINAGES. SITE PLANS ARE FOR GUIDANCE AND LAYOUT PURPOSES.
7. SEE CIVIL DRAWINGS FOR FINISH GRADE ELEVATIONS.
8. CONCRETE REINFORCE TO BE A MINIMUM OF #4 TIEBAR W/ TIEBAR JOINTS AT #4 G.C. ELEVATIONS/CONSTRUCTION JOINTS SHALL BE A MAXIMUM OF 60" MAX W/ 120" MAX. SLOPE DOWNWARD JOINTS TO HAVE COMPRESSIVE EXPANSION TIEBAR MINIMUM OF 1/4". SEE LANDSCAPE DRAWINGS FOR FINISH.
9. PAVES CURBS AND PROVIDE BASE TO FORM OF FIVE LINES AS REQUIRED BY SHADING THE SURFACE FINISHED 3'-0".
10. CONSTRUCTION DOCUMENTS PERTAINING TO THE LANDSCAPE AND IMPROVEMENT OF THE ENTIRE PROJECT SITE SHALL BE SUBMITTED TO THE BUILDING DEPARTMENT AND APPROVED BY PUBLIC FACILITIES DEVELOPMENT PRIOR TO ISSUANCE OF BUILDING PERMITS.
11. PRIOR TO FINAL CITY INSPECTION, THE LANDSCAPE ARCHITECT SHALL SUBMIT A CERTIFICATE OF COMPLETION TO PUBLIC FACILITIES DEVELOPMENT.
12. ALL LANDSCAPE AND IRRIGATION DESIGNS SHALL MEET CURRENT CITY STANDARDS AS LISTED IN ORDINANCES OR AS OBTAINED FROM PUBLIC FACILITIES DEVELOPMENT.
13. LANDSCAPED AREAS SHALL BE DELINEATED WITH A MINIMUM SIX INCHES (6") HIGH CURB.
14. APPROVED CONCEPTUAL LANDSCAPE PLAN PRIOR TO GRADING PERMIT.
15. PROVIDE RECESSED SIGN BOX AT ALL MAIN ENTRY DOORS.

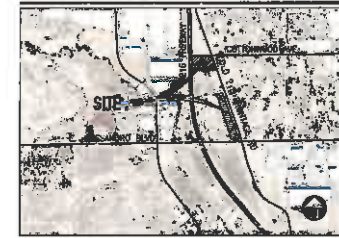
SITE LEGEND

- LANDSCAPED AREA
- AC PAVING - SEE "C" CIVIL DRAW FOR THICKNESS
- CONCRETE PAVING SEE "C" DRAWINGS FOR THICKNESS
- SEE CIVIL DRAW FOR FINISH GRADE ELEVATIONS. ROAD TO BE IDENTIFIED WITH FINISHED ROAD SURFACE AND MARKED "FIVE LANE-NO PARKING" PER CIVIL DRAW. REVERSE SIDE OF DRAW.
- POINT OF ADJACENT LADDER ROOF ACCESS
- WAY OF TRAVEL
- PROVIDE A SIGN AT EVERY ANGLE OF ACCESSIBLE PATH DISPLAYING THE INTERNATIONAL SYMBOL OF ACCESSIBILITY. SIGN SHALL INDICATE THE DIRECTION TO ACCESSIBLE FACILITY ENTRANCES AND COMPLY WITH 11197.6 THROUGH 11197.6.10
- SHADDED PAVING STALL (2' x 10')
- HANDICAP PARKING STALL (8' x 16')
- FUEL EFFICIENT VEHICLE STALL (8' x 16')
- C.V.P. CONTROL VEHICLE STALL (8' x 16')
- LIGHT SIGNING
- REMOVE FIRE WEAPON APPEARANCE LOCUS
- REMOVE FIRE WEAPON APPEARANCE LOCUS
- REMOVE FIRE WEAPON APPEARANCE LOCUS
- REMOVE FIRE WEAPON APPEARANCE LOCUS

Tabulation

SITE AREA	PHASE 3		PHASE 2										TOTAL
	Building B	Subtotal	Sublot 1	Sublot 2	Sublot 3	Sublot 4	Sublot 5	Sublot 6	Sublot 7	Subtotal	Street		
IN S.F.	88,248		72,165	44,093	27,108	28,044	48,344	61,515	70,714	352,312	13,827	107,238	611
IN ACRES	2.0		1.7	1.0	0.8	0.7	1.1	1.4	1.6	8.1	0.3	2.4	0.01
BUILDING AREA													
Office	2,500		1,000	1,000	1,000	1,000	1,000	1,000	4,000	10,000		10,000	
Warehouse	2,500		24,929	17,473	11,015	12,156	18,540	25,650	24,327	134,049		154,048	
TOTAL			25,929	18,473	12,015	13,156	19,540	26,156	28,327	144,049		164,048	
COVERAGE			36.0%	42.0%	44.3%	45.2%	40.4%	42.2%	40.1%	40.8%		38.2%	
AUTO PARKING PROVIDED													
Office	40		4	4	4	4	4	4	4	16	40	40	
Warehouse	133		26	18	12	13	19	25	25	138		133	
TOTAL			20	22	16	17	23	30	41	178		178	
AUTO PARKING PROVIDED													
Standard (8' x 18')	61		20	22	16	20	20	30	41	178		178	
MAXIMUM FLOOR AREA RATIO													
Office	100%												
Warehouse	1/1,000												
MAXIMUM BUILDING HEIGHT ALLOWED													
Office	40'												
Warehouse	40'												
ZONING ORDINANCE FOR CITY													
Zoning Designation	BMP												
SETBACK													
Front - Building over 30' High	40'												
Side - 0'													
Rear - 0'													

Aerial Map



HPA
INCORPORATED

FOR THE:
16331 BARSTOW AVENUE - SUITE #100
IRVINE, CA 92614
TEL: 949-233-1770
FAX: 949-859-0861
EMAIL: hpa@hpaonline.com

REV
WHEELS
BUSINESS PARK

PROJECT NO. 00000
DATE: 04/20/06

Project:
REV WHEELS BUSINESS PARK

Rev 000, CA

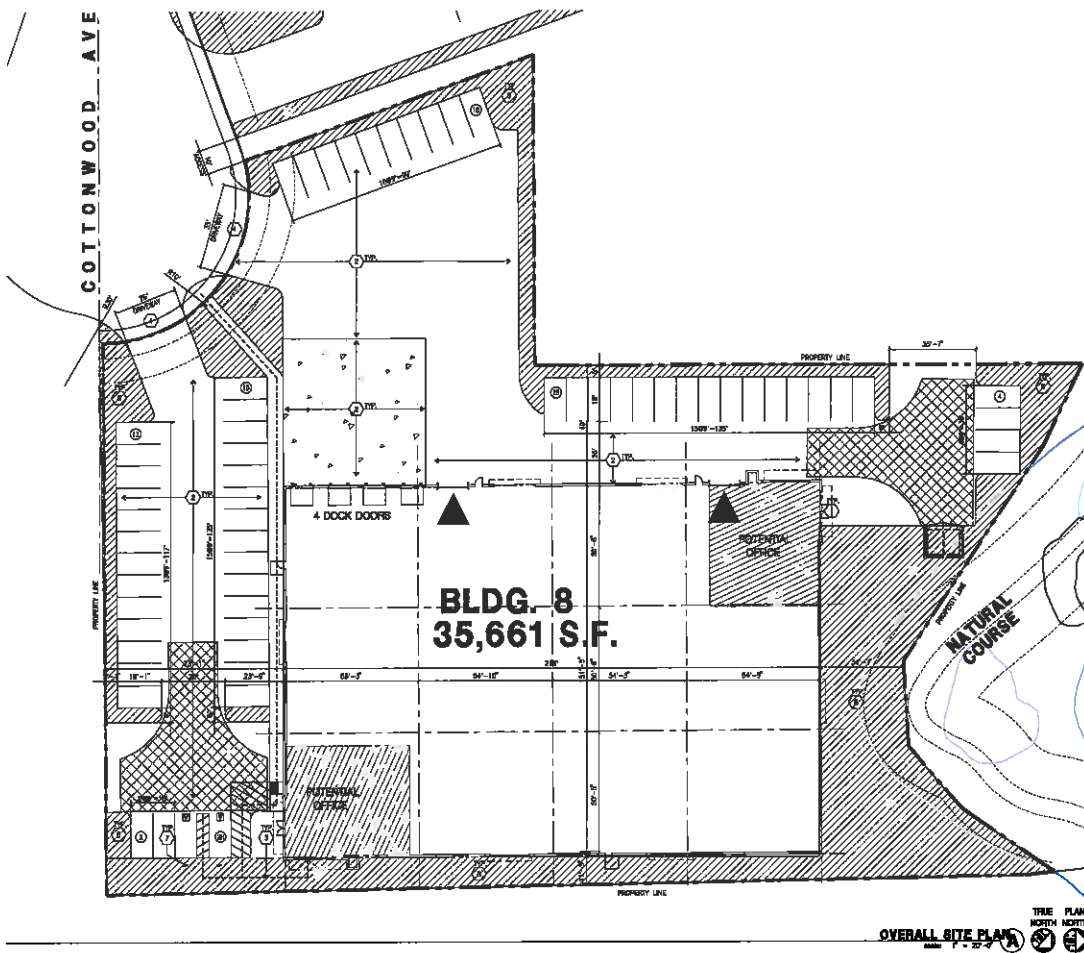
Consultants:

CIVIL ENGINEER: SON & ASSOC.
STRUCTURAL ENGINEER: KRAMER,
MECHANICAL ENGINEER: ACS
PLUMBING ENGINEER: JOHNSON
ELECTRICAL ENGINEER: HILL
LANDSCAPE ARCHITECT: HARTNER
FIRE PROTECTION ENGINEER: CARLSON
SOILS ENGINEER: NORCAL

Title: Overall site plan

Project Number: 15128
Drawn by: CC
Date: 08/05/2006
Revision:

Sheet
A1.1E



OVERALL SITE PLAN

SITE PLAN KEYNOTES

- 1 TRASH ENCLOSURE, PER CITY REQUIREMENTS.
- 2 ASPHALT CONCRETE (AC) PAVING, SEE CIVIL NOTES.
- 3 CONCRETE WALKWAY.
- 4 DRAINAGE AREAS PER CITY STANDARDS.
- 5 SIDE WALK.
- 6 PROVIDE METAL WALKWAY GRATED OVER 8" HIGH-PIG LOAD PER FIRE DEPARTMENT STANDARDS. DATE TO BE DESIGNED FOR 100 MPH WIND LOAD, EXP. "C".
- 7 HANDICAPPED PARKING STALL SIGN.
- 8 LANDSCAPE, SEE LANDSCAPE DESIGN LANDSCAPE AREAS INDICATED BY SHADDED PATTERNS.
- 9 PRE-CAST CONC. WALKWAY STOP.
- 10 DRIVE THRU DOCK RAMP.
- 11 HANDICAP ACCESSIBLE RAMP.

SITE PLAN GENERAL NOTES

1. ALL FOUNDATIONS ARE TO THE FACE OF CONCRETE WALL, FACE OF CONCRETE CURB OR DRIP LINE WALL.
2. SEE CIVIL PLANS FOR ALL CONCRETE CURBS, BUTTRES AND WALLS.
3. THE ENTIRE PROJECT SHALL BE PERMANENTLY MAINTAINED WITH AN AUTOMATIC WINDSHIELD WIPER PRIOR TO INSTALLATION & AT LEAST 90 DAYS BEFORE BEING COMPLETED.
4. SEE CIVIL DRAWINGS FOR POINTS OF CONNECTIONS TO OFF-SITE UTILITIES. CONTRACTOR SHALL VERIFY ACTUAL UTILITY LOCATIONS.
5. PROVIDE POSITIVE DRAINAGE AWAY FROM BUILDING. SEE CIVIL DRAWINGS.
6. CONTRACTOR TO REFER TO CIVIL DRAWINGS FOR ALL HORIZONTAL CURBING DIMENSIONS. SITE PLANS ARE FOR GENERAL AND STARTING LAYOUT POINTS.
7. SEE CIVIL DRAWINGS FOR FRESH GRADE ELEVATIONS.
8. CONCRETE REBAR SHALL BE A MINIMUM OF #4 TYPED JOINTS AT 4' O.C. TRANSVERSE REINFORCEMENT SHALL BE A MINIMUM 12" DIA. W/ 1/2" DIA. WELDED WIRE MESH. CONTRACTOR TO VERIFY COMPRESSIVE STRENGTH PLAN MATERIALS OF 1/4". SEE LANDSCAPE DRAWINGS FOR TREES.
9. TRAFFIC CURBS AND PROVIDE SIGNS TO INDICATE THE LOCATION AS REQUIRED BY CHICAGO FIRE DEPARTMENT STANDARD 9-01.
10. CONSTRUCTION DOCUMENTS PERTAINING TO THE LANDSCAPE AND PROVISION OF THE CIVIL PROJECT SITE SHALL BE SUBMITTED TO THE BUILDING DEPARTMENT AND APPROVED BY PUBLIC FACILITIES DEVELOPMENT PRIOR TO BEGINS OF BUILDING PERMITS.
11. PRIOR TO FINAL CITY INSPECTION, THE LANDSCAPE ARCHITECT SHALL SUBMIT A CERTIFICATE OF COMPLETION TO PUBLIC FACILITIES DEVELOPMENT.
12. ALL LANDSCAPE AND PROVISION DRAWINGS SHALL MEET CHICAGO CITY STANDARDS AS LISTED IN CUBES/BOOK AS OBTAINED FROM PUBLIC FACILITIES DEVELOPMENT.
13. LANDSCAPED AREAS SHALL BE DEMARCATED WITH A MINIMUM SIX INCHES (6") HIGH CURB.
14. APPROVED CONCEPTUAL LANDSCAPE PLAN FROM TO BEING POINT.
15. PROVIDE RECESSED WALK SIGN AT ALL MAIN ENTRY DOORS.

SITE LEGEND

LANDSCAPED AREA	STANDARD PARKING STALL (8' x 18')
AC PAVING - 2" THICK DRIVEWAY PER THROUGHWAYS	HANDICAP PARKING STALL (8' x 18')
CONCRETE PAVING 3" THICK DRIVEWAY PER THROUGHWAYS	FULL OFF-ROAD VEHICLE STALL (8' x 18')
8" HIGH PRE-APPROVED ACCESS ROAD, ROAD TO BE EXISTING	CARPOOL VEHICLE STALL (8' x 18')
PAINTED RED CURBS AND MARKED THE LAWN-TO PARKING PER CITY OF CHICAGO FIRE DEPT.	LIGHT ENCLOSURE
POINT OF AERIAL LADDER ROOF ACCESS.	PRIVATE FIRE HYDRANT - APPROPRIATE LOCATION
PATH OF TRAVEL.	WATER LINE - SEE CIVIL
	SEWER LINE - SEE CIVIL

Tabulation

PHASES	PHASE 2										Total			
	Building 1	Building 2	Building 3	Building 4	Building 5	Building 6	Building 7	Subtotal	Street	Other				
SITELAND														
in a.f.	69,248	81,515	70,714	353,312	13,027	347,236	a.f.							
in acres	2.0	2.4	2.0	10.1	0.4	9.7	acres							
BUILDING AREA														
Office	2,500	1,000	1,000	1,000	1,000	1,000	10,000							
Mazzena	2,800	24,925	17,473	11,915	16,540	25,690	24,307	154,048					134,048	
Warehouse	30,681	28,925	18,473	12,015	13,195	19,540	28,158	28,327	144,048					144,048
TOTAL	35,981	35.9%	42.0%	41.2%	46.2%	40.4%	42.2%	40.1%	40.8%			39.2%		
COVERAGE														
AUTO PARKING REQUIRED														
Office: 1/250 a.f.	4	4	4	4	4	4	18	40					40	
Warehouse: 1/1,000 a.f.	25	17	12	12	13	18	28	25	138					138
TOTAL	29	22	16	17	23	30	41	178					178	
AUTO PARKING PROVIDED														
Standard (8' x 18')	20	22	16	20	20	30	41	178					178	
MAXIMUM FLOOR AREA RATIO														
FAR-0.60														
MAXIMUM BUILDING HEIGHT ALLOWED														
Height - 45'														
ZONING ORDINANCE FOR CITY														
Zoning Designation - S-M2														
SETBACKS														
Front - Building over 30' High: 40'														
Building under 30' High: 20'														
Side - 0'														
Rear - 0'														

Aerial Map



HPA
HUMAN PERFORMANCE ASSOCIATES

1114, Inc.
18811 Barclay Avenue, Suite #100
Irvine, CA 92614
Tel: 949-485-1770
Fax: 949-485-0881
email: hpa@hpausa.com



1300X
X3000X
161300000

Project:
**REV WHEELS
BUSINESS PARK**

Rev 04a, CA

Consultants:

- | | |
|--------------|--------------|
| CITY & CIVIL | SON & ASSOC. |
| STRUCTURAL | KRAMER |
| MECHANICAL | ACS |
| PLUMBING | JOHNSON |
| ELECTRICAL | TAL |
| LANDSCAPE | HUNTER |
| PREPARED | CARDIN |
| SUBSEQUENT | NORDAL |

Title: Overall site plan

Project Number: 18125
Drawn by: CC
Date: 08/05/2018
Revision:

Sheet:
A1.1F



HIP, Inc.
16311 Harbor Drive, - #6, P.O.
Irvine, CA
92614
Tel: 949-853-7770
Fax: 949-853-0851
email: hip@hipinc.com



XXXXX
XXXXXX
XXXXXX

Project:

REV WHEELS
BUSINESS PARK

Irvine, CA

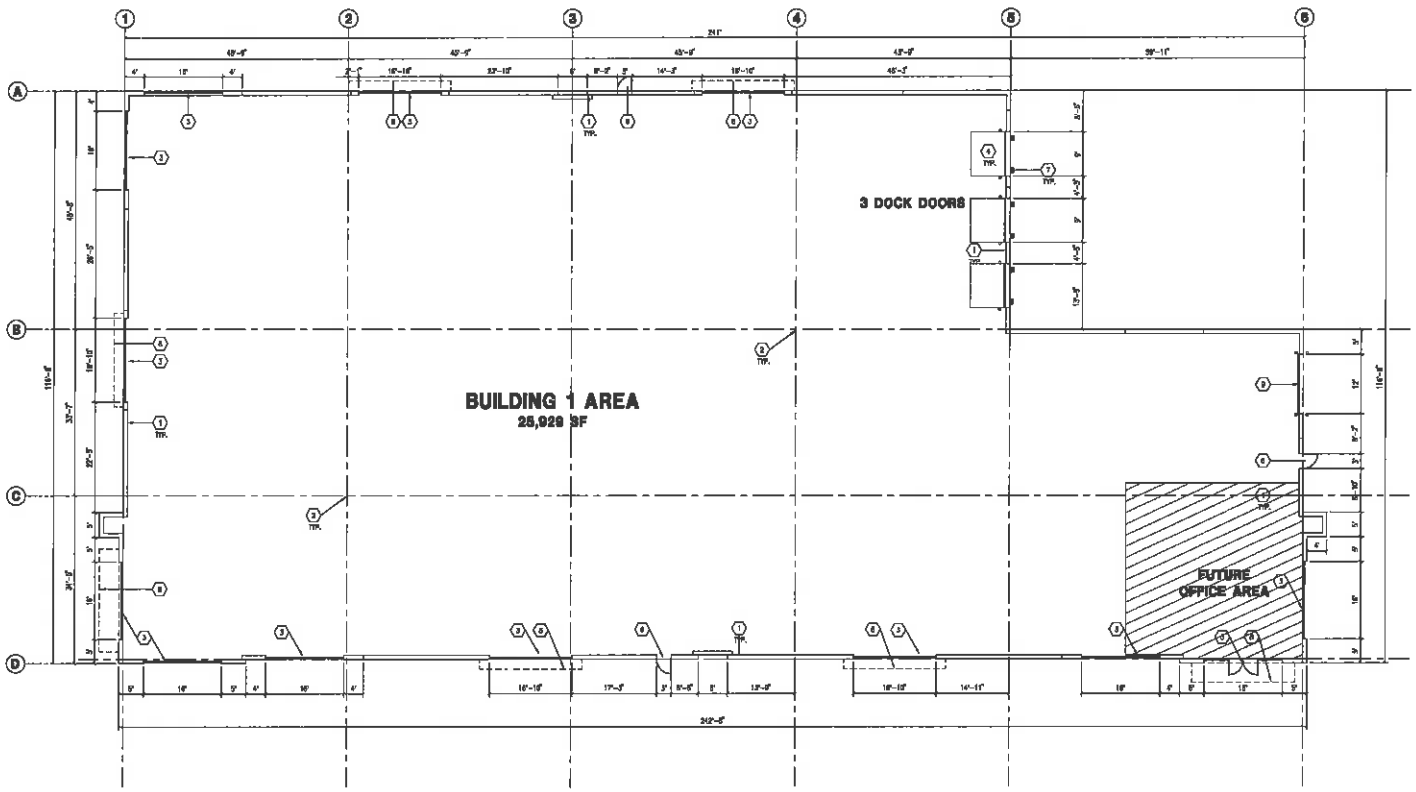
Consultants:

CIVIL	SDH & ASSOC.
STRUCTURAL	KRAMER,
MECHANICAL	AGS
PLUMBING	JOHNSON
ELECTRICAL	WALL
LANDSCAPE	HARTER
FIREPROTECTOR	CAFSON
SUBMITTER	MORALE

Title: Overall floor plan

Project Number: 18189
Drawn by: CC
Date: 06/15/2018
Revision:

Sheet
A2.1



KEYNOTES - FLOOR PLAN

- ① CONCRETE TILT-UP PANEL.
- ② STRUCTURAL STEEL COLUMN.
- ③ TYPICAL WOODWORKING BEARING WITH GLAZING, SEE ELEVATIONS FOR SILL, SOULS AND LOCKERS.
- ④ 8'-0" x 10' TRUCK DOOR, SECTIONAL OPL, STANDARD GANGE, DESIGNED TO RESIST WIND TO 140MPH, EXPOSURE 'C'.
- ⑤ 8'-0" x 5'-0" x 4" THICK CONCRETE EXTERIOR LANDING PAD TYPICAL AT ALL EXTERIOR MAIN DOORS TO LANDSCAPED AREA, THESE TO BE MATCHED BRIDGE WIDTH, SLOPE TO BE 1/4" / 1' MIN. PROVIDED EQUAL TO SLOPE DRAINAGE FOR CITY REQUIREMENTS. JOIST FOLLOW BRIDGE, EXTERIOR MAIN DOOR, DESIGNED TO RESIST WIND TO 140MPH, EXPOSURE 'C'.
- ⑥ DOOR DOOR BUMPER.
- ⑦ METAL CHIMNEY ABOVE.
- ⑧ SEE 1/4" BRIDGE TRAIL, SECTIONAL OPL, STANDARD GANGE, DESIGNED TO RESIST WIND TO 140MPH, EXPOSURE 'C'.

GENERAL NOTES - FLOOR PLAN

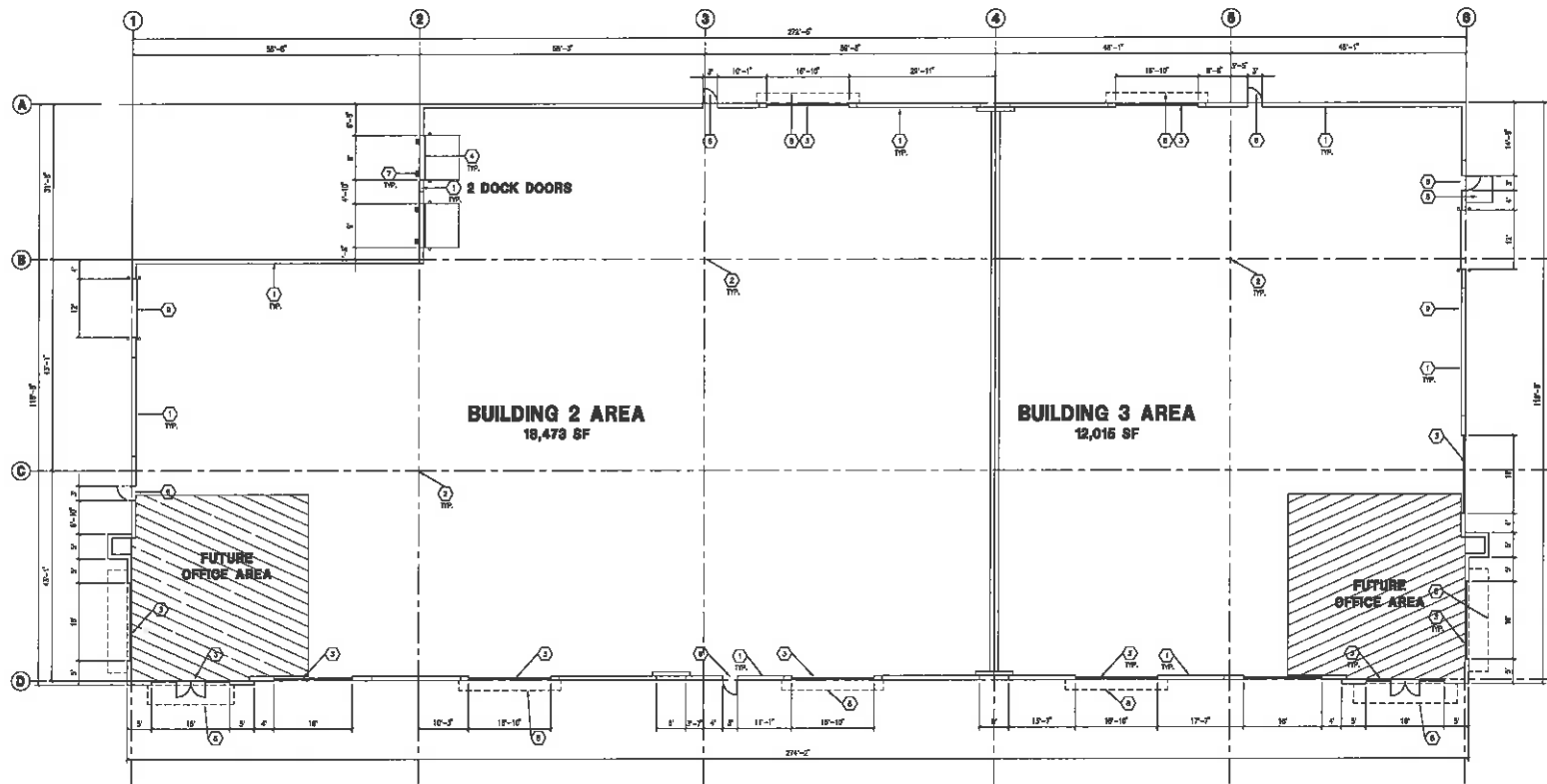
- A. THIS BUILDING IS DESIGNED FOR WIND FILE STORMS WITH THE ACCESS MAIN DOORS AT 100 MPH WIND. A SEPARATE PERMIT WILL BE REQUIRED FOR ANY PROHIBITIVE SYSTEMS.
- B. FIRE HOSE LOCATIONS SHALL BE APPROVED FOR FIRE DEPARTMENT.
- C. THE BUILDING FLOOR SLAB IS FLAT, SEE 'C' DRAWINGS FOR FINISH SURFACE ELEVATIONS.
- D. WAREHOUSE INTERIOR CONCRETE WALLS ARE FINISHED WHITE COLLARED AND TO RECEIVE FINISH ONLY. ALL DOOR, WINDOW WALLS IN WAREHOUSE TO RECEIVE 1 COAT OF WHITE TO COVER.
- E. SLOPE FOUR STEP 1/2" TO EXTERIOR AT ALL WAREHOUSE DOORS, SEE 'C' DRAWINGS FOR MAIN STEP LOCATIONS.
- F. ALL DIMENSIONS ARE TO THE FACE OF CONCRETE PANEL, WALL, BRIDGING, OR FACE OF STEEL I-BEAM.
- G. SEE CALL DIMENSIONS FOR POINT OF CONNECTIONS TO OFF-SITE UTILITIES, CONTRACTOR TO VERIFY ACTUAL UTILITY LOCATIONS.
- H. CONTRACTOR TO VERIFY AND KEEP THE FLOOR SLAB CLEAR, ALL EQUIPMENT TO BE DIMENSIONED INCLUDING CURBS AND TRACKS.

PAINT NOTES

- 1. INTERIOR CONCRETE WALL: SINGLE COAT OF ACRYLIC PAINT ON INTERIOR WALLS.
- 2. ALL STRUCTURAL STEEL: LIGHT GRAY SHOP PRIMER, MEMBER END COATING, EXTERNAL JOINTS, & EXTERIOR.

OVERALL FLOOR PLAN
DATE: 6-15-18





OVERALL FLOOR PLAN
 TRUE NORTH
 SCALE 1" = 12'-0"

GENERAL NOTES - FLOOR PLAN

- A. THIS BUILDING IS DESIGNED FOR HIGH PILE STORAGE WITH FIRE ACCESS FROM ROADS. IF FIRE WAREHOUSE, A SEPARATE PERMIT WILL BE REQUIRED FOR ANY WAREHOUSE/CONVEYOR SYSTEMS.
- B. FIRE HOSE LAYOUTS SHALL BE APPROVED FOR FIRE DEPARTMENT.
- C. THE BUILDING FLOOR SLAB IS FIN. SEE "C" DIMENSIONS FOR FINISH SURFACE ELEVATIONS.
- D. WAREHOUSE INTERIOR CONCRETE WALLS AND PAINTED WHITE COLUMNS ARE TO RECEIVE FINISH ONLY. ALL OTHER WALLS IN WAREHOUSE TO RECEIVE 1 COAT OF WHITE TO COLOR.
- E. BUMP FOUR STEP UP TO EXTERIOR AT ALL WAREHOUSE ENDS. SEE "E" DIMENSIONS FOR FOUR STEP LOCATION.
- F. ALL DIMENSIONS ARE TO THE FACE OF CONCRETE PANEL, WALL, OR COLUMN, UNLESS NOTED OTHERWISE.
- G. SEE CIVIL DRAWINGS FOR POINT OF CONNECTIONS TO OFF-SITE UTILITIES. CONTRACTOR TO VERIFY ACTUAL UTILITY LOCATIONS.
- H. CONTRACTOR TO PROTECT AND KEEP THE FLOOR SLAB CLEAN. ALL DEBRIS TO BE REMOVED INCLUDING CHIPS AND BRACKS.

KEYNOTES - FLOOR PLAN

- 1 CONCRETE FLE-UP PANEL.
- 2 STRUCTURAL STEEL COLUMN.
- 3 TYPICAL STRUCTURE SYSTEM WITH GLAZING. SEE ELEMENTARY FOR SIZE, COLOR AND LOCATION.
- 4 8'-0" x 8'-0" TRUSS DOOR. REGIONAL CYS. SPURRING OFFICE. COLORED TO MATCH W/IN 30% EXPOSURE "C".
- 5 2'-0" x 2'-0" x 4'-0" TRUSS CONCRETE. EXTERIOR FINISH. FINISH TO BE MATCH FROM FINISH FLOOR TO BE 1/4" 1/2" DIA. PROVIDE WALK TO HARD SURFACE FOR CITY REQUIREMENTS.
- 6 30" HOLLOW METAL EXTERIOR W/IN DOOR. COLORED TO MATCH W/IN 30% EXPOSURE "C".
- 7 DOCK DOOR NUMBER.
- 8 METAL CLAMP ABOVE.
- 9 22" x 14" DRIVE SHAFT. SECTION ON. STANDARD GRADE. DESIGNED TO RESIST W/IN 60 MPH. EXPOSURE "C".

PAINT NOTES

1. INTERIOR CONCRETE WALL: 100% 200% 40% 60% 80% 100% PAINT OR INTERIOR WALLS.
2. ALL STRUCTURAL STEEL, WAREHOUSE AND COLUMNS, EXTERIOR METAL, & DRIVE.



H.P.A. Inc.
 14381 DANFORD AVENUE, - SUITE #100
 WHEELS, GA 30092
 TEL: 949-553-1770
 FAX: 949-553-0851
 EMAIL: hpa@hpa-inc.com



100000
 200000
 300000

Project:
REV WHEELS BUSINESS PARK

Wayville, GA

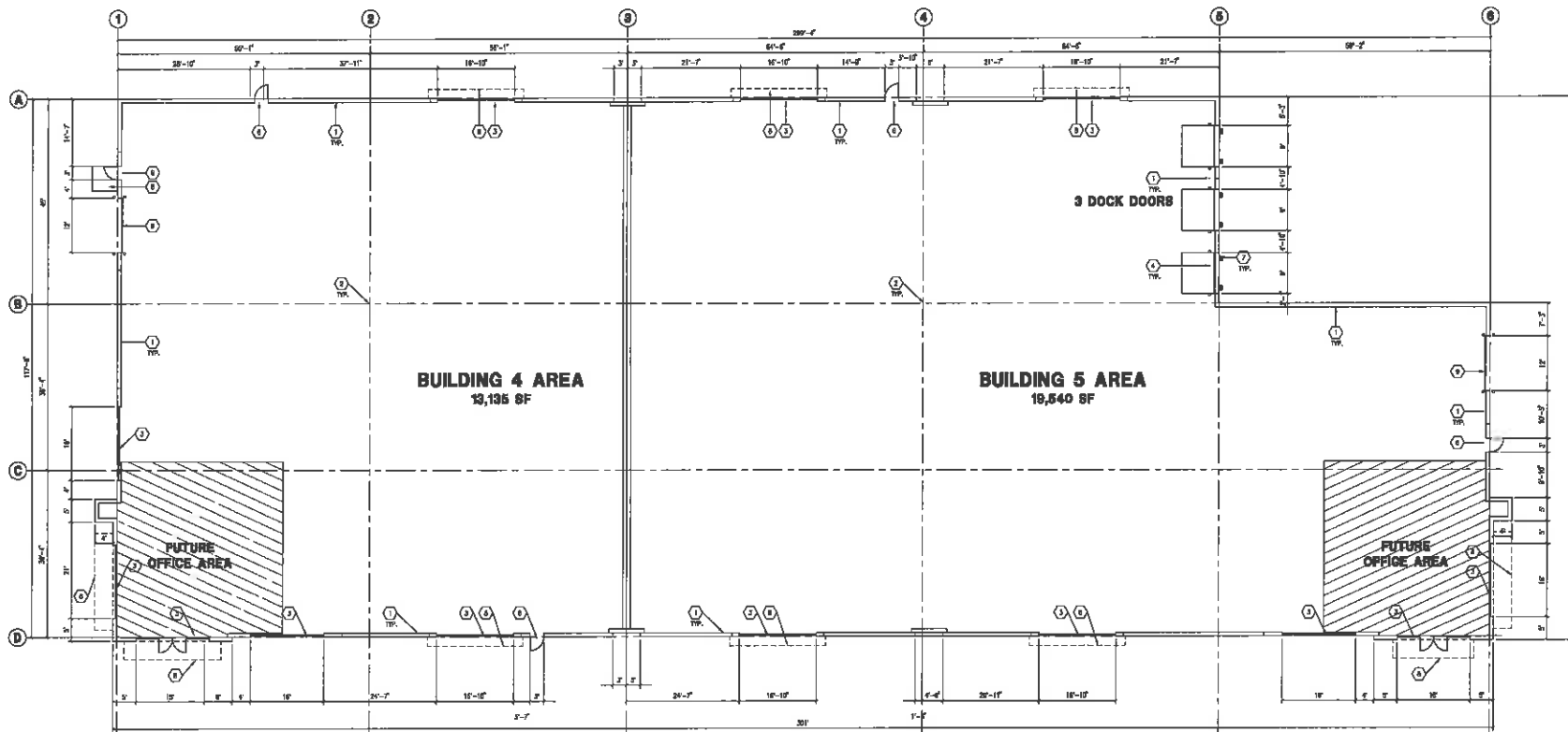
Consultants:

CIVIL	SDM & ASSOC.
STRUCTURAL	KRAMER
MECHANICAL	AGIS
PLUMBING	JOHNSON
ELECTRICAL	HAL
LANDSCAPE	HUNTER
FIRE PROTECTION	CARDON
SOIL REMEDIATION	NORCAL

Title: Overall floor plan

Project Number: 15129
 Drawn by: CC
 Date: 08/15/2016

Sheet:
A2.2



OVERALL FLOOR PLAN
 TITLE NORTH PLAN NORTH
 SCALE: 1/8" = 1'-0"

KEYNOTES - FLOOR PLAN

- ① CONCRETE 1/2" THK PANEL.
- ② STRUCTURAL STEEL COLUMN.
- ③ TYPICAL SPRINKLER SYSTEM WITH GLASS GLOBE ELVATIONS FOR WDG. COLUMN AND LOCATIONS.
- ④ 2'-0" x 1'-0" TRUSS DOOR, SECTIONAL OIL, STANDARD GRADE, DESIGNED TO RESIST WIND 90 MPH, EXPOSURE "C".
- ⑤ 2'-0" x 9'-0" x 3'-0" WIND RESISTANT DOORWAY LAMINA PER TYPICAL AT ALL DOORWAY MAN DOORS TO UNLOCKED PANEL, FRUSH TO BE 1/2" MIN. BRUSH FRUSH SLACK TO BE 1/2" x 1/2" MAX. PROVIDE WAX TO WIND SURFACE PER CITY REQUIREMENTS.
- ⑥ 3'-0" RAILING METAL OUTDOOR MAN DOOR, DESIGNED TO RESIST WIND 90 MPH, EXPOSURE "C".
- ⑦ DOOR SWAGER.
- ⑧ METAL CANOPY ABOVE.
- ⑨ 22" x 14" DRIVE TRAIL, SECTIONAL OIL, STANDARD GRADE, DESIGNED TO RESIST WIND 90 MPH, EXPOSURE "C".

GENERAL NOTES - FLOOR PLAN

- A. THIS BUILDING IS DESIGNED FOR HIGH PILE STORAGE WITH FIRE PROTECTIVE MAN DOORS AS THE MAXIMUM. A SEPARATE PERMIT WILL BE REQUIRED FOR ANY PACKING/UNPACKING SYSTEMS.
- B. FIRE HOSE LOCATIONS SHALL BE APPROVED FOR FIRE DEPARTMENT.
- C. THE BUILDING FLOOR SLAB IS FINI, SEE "1" DRAWINGS FOR FINISH SURFACE ELEVATIONS.
- D. WAPROPOSE EXTERIOR CONCRETE WALLS ARE PAINTED WHITE, COLUMNS TO 10' HEIGHT CLAD IN STEEL BR. SLABS IN FINISHWORK.
- E. SLOPE FOUR (4) INCHES TO ONE (1) INCH AT ALL WINDOON EATLS, SEE "1" DRAWINGS FOR FOUR (4) INCHES TO ONE (1) INCH.
- F. ALL DIMENSIONS ARE TO THE FACE OF CONCRETE PANEL, WALL, CORNER, OR FACE OF STEEL U-LAG.
- G. SEE CIVIL DRAWINGS FOR POINT OF CONNECTIONS TO OFF-SITE UTILITIES, CONNECTIONS TO VERIFY ACTUAL UTILITY LOCATIONS.
- H. CONTRACTOR TO VERIFY AND KEEP THE FLOOR SLAB CHECK. ALL EQUIPMENT TO BE SHIPPED INCLUDING GEAR AND TRUCKS.

PAINT NOTES

- 1. EXTERIOR CONCRETE WALL: SINGLE COAT OF ACRYLIC PAINT ON EXTERIOR WALLS.
- 2. ALL STRUCTURAL STEEL MEMBER AND COLLARS, BRACKET, WELLS, & CHANNELS: LIGHT GRAY SHOP PRIMER.



HPA, Inc.
 18851 badami avenue, - Ste. #100
 Irvine, CA 92612
 tel: 949-852-1770
 fax: 949-852-0851
 email: hpa@hpa.com



XXXXX
 XXXXXX
 tel: 213.333.3333

Project:
REV WHEELS BUSINESS PARK

Revista, CA

Consultants:

CIVIL	SDH & ASSOC.
STRUCTURAL	KRAMER,
MECHANICAL	AGS
PLUMBING	JOHNSON
ELECTRICAL	HYL
LANDSCAPE	HUNTER
FIRE/PROTECTIN	CARLON
SCALE/ENGINEER	MORCAL

Title: Overall floor plan

Project Number: 16128
 Drawn by: CC
 Date: 08/06/2018
 Revisions:

Sheet:
A2.3



HPA, Inc.
18831 Gordon Avenue, - 949-9100
Irvine, CA 92612
Tel: 949-953-1770
Fax: 949-953-0851
Email: hpa@hpa.com



XXXXX
XXXXX
REV:XXXXX

Project:
**REV WHEELS
BUSINESS PARK**

Rev 06/04

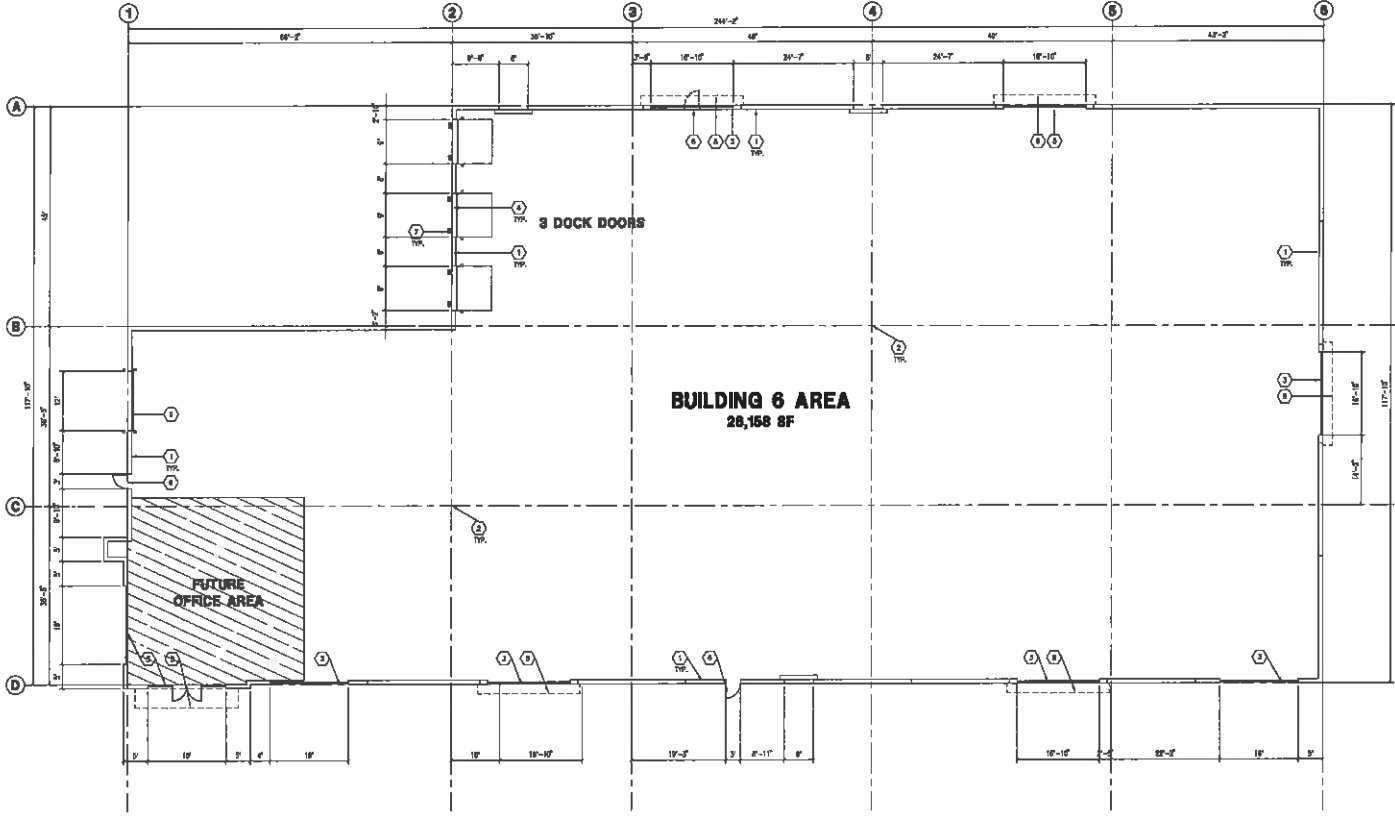
Consultants:

CIVIL STRUCTURAL MECHANICAL PLUMBING ELECTRICAL LANDSCAPE PREPARED BY SOLE ENGINEER	GOM & ASSOC. KRAMER, ACS JOHNSON HALL HUNTER CARLIN MORFAL
--	---

Title: Overall floor plan

Project Number: 16129
Drawn by: CC
Date: 06/05/2016
Revision:

Sheet:
A2.4



KEYNOTES - FLOOR PLAN

- 1 CONCRETE FILL-UP PANEL.
- 2 STRUCTURAL STEEL COLUMN.
- 3 TYPICAL STRENGTHENMENT SYSTEM WITH BRACING, SEE ELEVATIONS FOR SIZES, COLORS AND LOCATIONS.
- 4 8" x 4" x 1/2" TRUSS ROD, SECTIONAL, ENCL. STANDARD GRADE, DESIGNED TO RESIST 8K TO 8PKL, EXPOSURE "C".
- 5 2" x 4" x 1/4" x 4" TRUSS ROD, SECTIONAL, ENCL. STANDARD GRADE, DESIGNED TO RESIST 8K TO 8PKL, EXPOSURE "C".
- 6 2" x 4" x 1/4" x 4" TRUSS ROD, SECTIONAL, ENCL. STANDARD GRADE, DESIGNED TO RESIST 8K TO 8PKL, EXPOSURE "C".
- 7 2" x 4" x 1/4" TRUSS ROD, SECTIONAL, ENCL. STANDARD GRADE, DESIGNED TO RESIST 8K TO 8PKL, EXPOSURE "C".
- 8 DOOR CODE BARRIER.
- 9 METAL GARAGE ABOVE.
- 10 22" x 14" TRUSS ROD, SECTIONAL, ENCL. STANDARD GRADE, DESIGNED TO RESIST 8K TO 8PKL, EXPOSURE "C".

GENERAL NOTES - FLOOR PLAN

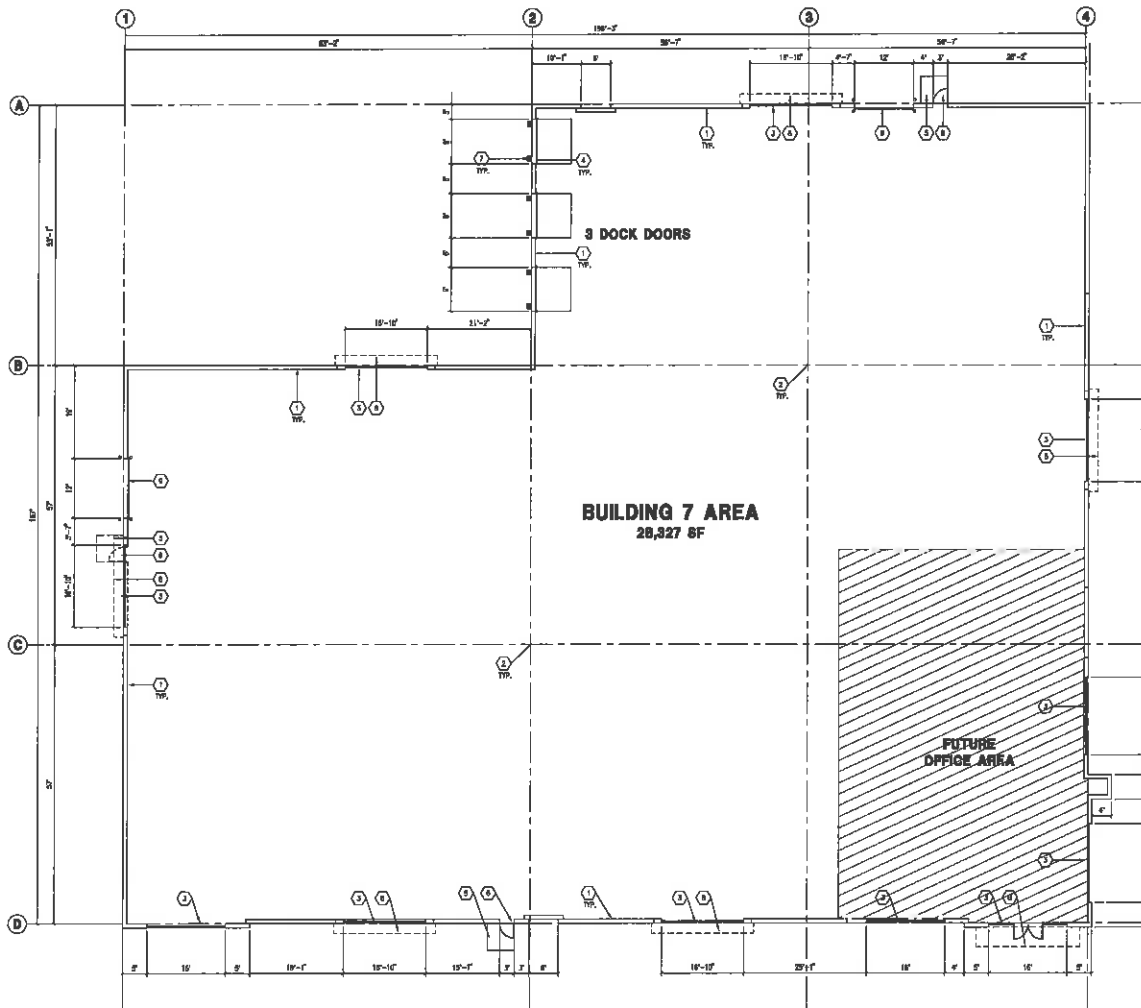
- A. THIS BUILDING IS DESIGNED FOR HIGH PILE STORAGE WITH BUSH ACCESS AND COVER BY THE MAINWAY. A SEPARATE PAVEMENT SHALL BE REQUIRED FOR ANY VEHICLES/CONCRETE TRUCKS.
- B. FIRE HOSE LOCATIONS SHALL BE APPROVED FOR FIRE DEPARTMENT.
- C. THE BUILDING FLOOR SLAB IS IN PLACE, SEE "C" DRAWING FOR FRESH SURFACE FINISHING.
- D. UNPROTECTED EXTERIOR CONCRETE WALLS ARE FINISHED WHITE COLOR AND TO BE COVERED WITH ONLY ALL DRY MIX WALL IN BRICKWORK TO PROVIDE 1 COAT OF WHITE TO COVER.
- E. SLOPE FROM STAIR UP TO PROVIDE AT ALL HANDOVER EXITS. SEE "E" DRAWING FOR FOUR STAIR LOCATIONS.
- F. ALL DIMENSIONS ARE TO THE FACE OF CONCRETE PANEL SHALL CORNER, ON FACE OF STEEL WALL.
- G. SEE CIVIL DRAWING FOR PLANT OF CONNECTIONS TO OFF-SITE UTILITIES. COORDINATION TO VERIFY ACTUAL UTILITY LOCATIONS.
- H. CONTRACTOR TO PROTECT AND KEEP THE FLOOR SLAB CLEAR. ALL CONTRACTOR TO BE EMPLOYED INCLUDING CURB AND TRUCKS.

PAINT NOTES

- 1. INTERIOR CONCRETE WALL: SINGLE COAT OF ACRYLIC PRIME ON INTERIOR WALLS.
- 2. ALL STRUCTURAL STEEL MEMBER: PRIME PAINT, ENCL. STANDARD GRADE, EXPOSURE "C".

OVERALL FLOOR PLAN
DATE: 6/5/16





KEYNOTES - FLOOR PLAN

- ① CONCRETE FLY-UP PANEL.
- ② STRUCTURAL STEEL COLUMN.
- ③ TYPICAL STEPPED ROSTER WITH GLAZING. SEE ELEVATIONS FOR RISE, COLOR AND LOCATION.
- ④ 8" OF 3" 10" THICK CONCRETE SECTIONAL WALL, STANDARD GRADE. DESIGNED TO RESIST 90 PSF, EXPOSURE "C".
- ⑤ 2'-0" x 2'-0" x 4" THICK CONCRETE EXTENDED LANDING FOR TYPICAL AT ALL EXTERIOR BAY DOORS TO LANDSCAPED AREA. FRESH TO BE CLEAN BRUSH FRESH PAINT TO BE 1/2" 1/2" MAX. PROVIDE SILL TO HOLD SWEEPING FLY-CRY PREVENTION.
- ⑥ 2"X2" HOLDING METAL SECTION WITH DOOR. DESIGNED TO RESIST 90 PSF, EXPOSURE "C".
- ⑦ DOCK DOOR BUMPER.
- ⑧ METAL CANOPY ABOVE.
- ⑨ 2" x 14" 20# STEEL THINWALL SECTIONAL GIL. STANDARD GRADE. DESIGNED TO RESIST 90 PSF, EXPOSURE "C".

GENERAL NOTES - FLOOR PLAN

- A. THIS BUILDING IS DESIGNED FOR HIGH PILE STORAGE WITH HIGH ACCESS BAY DOORS AT 150' MAXIMUM GLE. A SEPARATE POINT WILL BE REQUIRED FOR ANY NONSTANDARD ACCESS.
- B. FIRE HOSE LOCATIONS SHALL BE APPROVED PER FIRE DEPARTMENT.
- C. THE BUILDING FLOOR SLAB IS FLAG. SEE "C" DRAWINGS FOR FRESH SURFACE FINISHING.
- D. WAREHOUSE INTERIOR CONCRETE WALLS ARE PAINTED WHITE. COLUMN ARE TO BECOME FINISH ONLY. ALL DRY-ROD WALLS IN BAYHOUSE TO RECEIVE 1 COAT OF WHITE TO COVER.
- E. SLOPE FOUR STEPS 1/2" TO EXTERIOR AT ALL BAY DOOR EXITS. SEE "E" DRAWINGS FOR MORE STEP LOCATIONS.
- F. ALL DIMENSIONS ARE TO THE FACE OF CONCRETE PANEL WALL, COLUMN, OR FACE OF STEEL WALK.
- G. SEE CIVIL DRAWINGS FOR POINT OF CONNECTIONS TO OFF-SITE UTILITIES. COORDINATION TO VERIFY ACTUAL UTILITY LOCATIONS.
- H. CONTRACTOR TO PROTECT AND KEEP THE FLOOR SLAB CLEAN. ALL EQUIPMENT TO BE DAMPPED INCLUDING CURBS AND WELDS.

PAINT NOTES

- 1. INTERIOR CONCRETE WALL: SINGLE COAT OF ACRYLIC PRIMER ON INTERIOR WALLS.
- 2. ALL STRUCTURAL STEEL: LIGHT GRAY SHOP PRIMER, MEMBER END CONNECTIONS, BOLTS, JOISTS, & DECKING.

OVERALL FLOOR PLAN
DATE: 7-12-10
 THE PLAN NORTH

HPA
 HILLTOP PROJECTS
 Inc., Inc.
 1881 Jordan Avenue, #10, #100
 Irvine, CA 92614
 Tel: 949-483-1773
 Fax: 949-483-0951
 email: hpa@hilltop.com



XXXX
 X:XXXX
 No: XXXX

Project:
REV WHEELS BUSINESS PARK

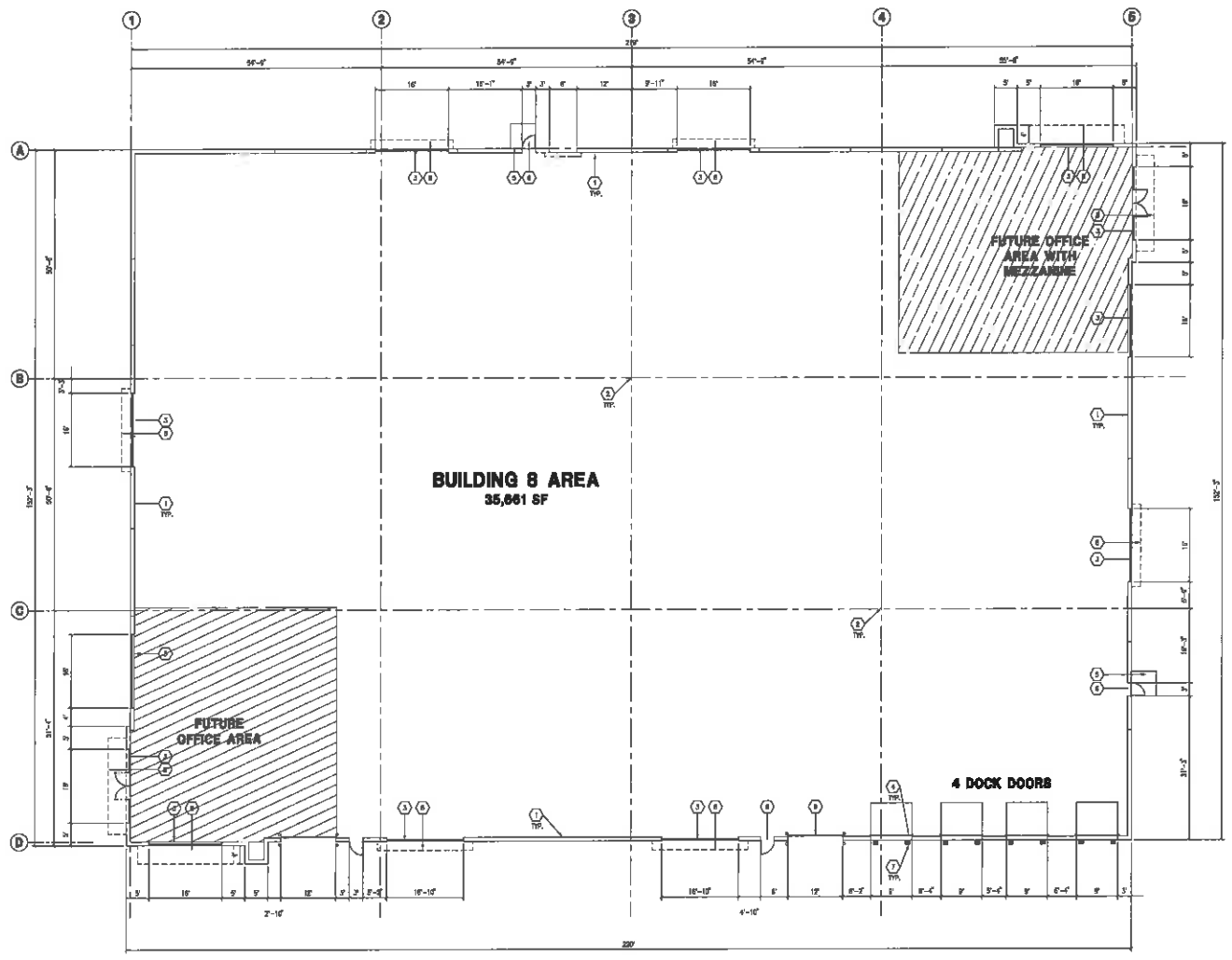
Irvine, CA

Consultants:
 CIVIL: SRM & ASSOC.
 STRUCTURAL: KRUMER
 MECHANICAL: AGS
 PLUMBING: JOHNSON
 ELECTRICAL: TML
 LANDSCAPE: HUNTER
 FIRE PROTECTION: CARRON
 SOIL ENGINEER: MORICAL

Title: Overall floor plan

Project Number: 18128
 Drawn by: CC
 Date: 08/25/10
 Revision:

Sheet
A2.5



KEYNOTES - FLOOR PLAN

- ① CONCRETE 1.5" UP PAINT.
- ② STRUCTURAL STEEL COLLARS.
- ③ TYPICAL REINFORCEMENT SYSTEM WITH CLADDING. SEE ELEVATIONS FOR REIN. COLOR AND LOCATIONS.
- ④ 8" OF 2" X 10" TRUSS DOOR, SECTIONAL, OIL, STAINLESS BRASS. DESIGNED TO RESIST WIND 90 MPH, EXPOSURE "C".
- ⑤ 8" OF 2" X 8" OF 2" X 4" TRUSS CONCRETE EXTERIOR CLADDING AND TYPICAL AT ALL EXTERIOR WALL JOINTS TO LAPPED AND FUSED TO BE HEAVY BRASS FRESH. SLUGS TO BE 1/4" X 1/4" X 1/4" W/6. PROVIDE 1/4" TO HARD SURFACE PER CITY REGULATIONS.
- ⑥ 3" X 1" HOLLOW METAL EXTERIOR WIND DOOR. DESIGNED TO RESIST WIND 90 MPH, EXPOSURE "C".
- ⑦ DOOR DOOR BUMPER.
- ⑧ METAL CANOPY ABOVE.
- ⑨ 2X 12" DRIVE TRAIL, SECTIONAL OIL, STAINLESS BRASS. DESIGNED TO RESIST WIND 90 MPH, EXPOSURE "C".

GENERAL NOTES - FLOOR PLAN

- A. THIS BUILDING IS DESIGNED FOR HIGH PILE STORAGE WITH FIVE ACCESS AND JOCKS AT THE REAR END. A SERVICE POINT WILL BE REQUIRED FOR ANY RIGGING/CONVEYER SYSTEM.
- B. FIRE HOSE LOCATIONS SHALL BE APPROVED PER FIRE DEPARTMENT.
- C. THE BUILDING FLOOR SLAB IS FLAT 2X12" DRAWING FOR FRESH REINFORCEMENT.
- D. REINFORCEMENT ANCHOR CONCRETE WALLS ARE PAINTED WHITE. COLLARS ARE TO BE PAINTED WHITE. ALL CONCRETE SHALL BE APPROVED TO RECEIVE 1 COAT OF WHITE TO COVER.
- E. SLABE FOUR STEPS EAST TO EXTERIOR AT ALL WINDOOR ENDS. SEE "E" DRAWING FOR FOUR STEP LOCATION.
- F. ALL DIMENSIONS ARE TO THE FACE OF CONCRETE PANEL, WALL, GROUND, OR FACE OF STEEL WALL.
- G. SEE CIVIL DRAWINGS FOR POINT OF CONNECTIONS TO DRY-PILE UTILITIES. CONTRACTOR TO VERIFY ACTUAL UTILITY LOCATIONS.
- H. CONTRACTOR TO VERIFY AND KEEP THE FLOOR SLAB BEARING. ALL EQUIPMENT TO BE EXPLODED INCLUDING DATA AND TRACKS.

PAINT NOTES

- 1. INTERIOR CONCRETE WALL - SINGLE COAT OF ACRYLIC PAINT ON INTERIOR WALLS.
- 2. ALL STRUCTURAL STEEL MEMBER, I.C. COLLARS, EXPOSED JOINTS, & BRACKETS.

OVERALL FLOOR PLAN
 DATE: 1-19-20
 PLAN NORTH
 NORTH

HPA
 CONSULTANTS

Hpa, Inc.
 1831 DORSET AVENUE - SUITE #100
 FOLSOM, CA 95632
 TEL: 916-983-1770
 FAX: 916-983-1881
 EMAIL: hpa@hpaconsultants.com



PROJECT
 J3000X
 J3000X
 ME 3100000

Project:
REV WHEELS BUSINESS PARK

Florida, CA

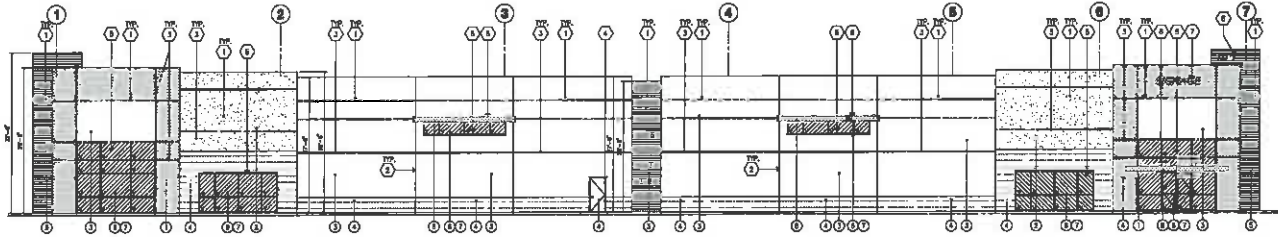
Consultants:

CIVIL	SDH & ASSOC.
STRUCTURAL	KRAMER
MECHANICAL	ACS
PLUMBING	JOHNSON
ELECTRICAL	WHL
LANDSCAPE	HUNTER
PROTECTION	CARBON
SOILS ENGINEER	MORCAL

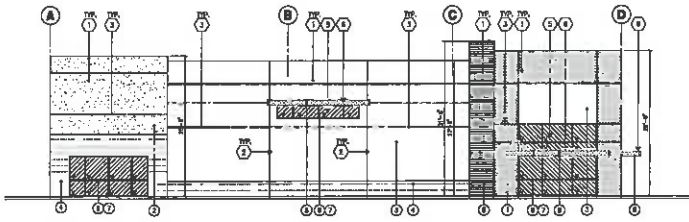
This: Overall floor plan

Project Number: 16129
 Drawn by: CC
 Date: 05/05/2016
 Revisions:

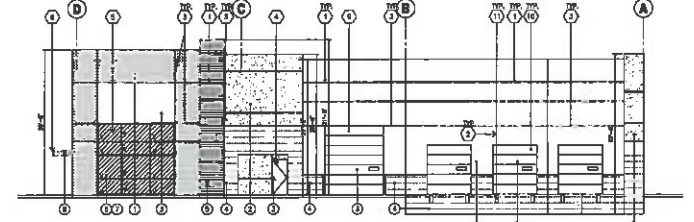
Sheet
A2.6



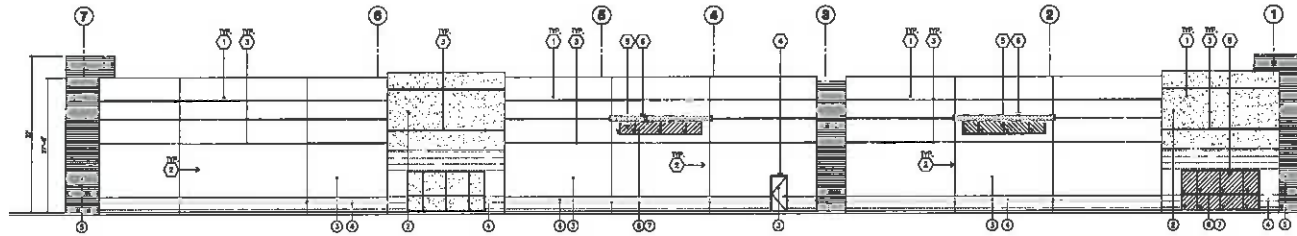
BUILDING 1 - EAST ELEVATION
SCALE: 1/8"=1'-0" (A)



BUILDING 1 - SOUTH ELEVATION
SCALE: 1/8"=1'-0" (C)



BUILDING 1 - NORTH ELEVATION
SCALE: 1/8"=1'-0" (B)



BUILDING 1 - WEST ELEVATION
SCALE: 1/8"=1'-0" (D)

GENERAL NOTES - SCREEN WALLS

- A. CONTRACTOR TO VERIFY ALL SCREENS MEET CURRENT CODE PLUMB & VENT SET CONDITIONS. VERIFY ALL TOP OF WALL FINISH (T.O.F.) IS FROM SURFACE FINISH.
- B. FINISH OF T.O.F. SHALL BE 1/2" MIN. IN SCREEN WALLS & 1/4" MIN. IN OTHER WALLS.
- C. CONTRACTOR TO PROVIDE WEIR-HIGHNESS ADJUSTED TO SCREEN WALL & ALL RETURN / EXHAUST CONDITIONS. PROVIDE WEIR-HIGHNESS OF 1/4" MIN. OF WALL TO CONTRACTOR TO VERIFY WEIR-HIGHNESS ADJUSTMENT TO ADJUSTMENT FROM WEIR-HIGHNESS.
- D. SCREEN WALLS TO BE SUBJECT TO WEIR-HIGHNESS ADJUSTMENT FROM WEIR-HIGHNESS ADJUSTMENT TO ADJUSTMENT FROM WEIR-HIGHNESS.
- E. USE MANUFACTURER TO PROVIDE DETAILS OF GATE TO WEIR-HIGHNESS AND WEIR-HIGHNESS FOR EACH WEIR-HIGHNESS. CONSULT "C" AND VERIFY ALL WEIR-HIGHNESS, DETAILS AND SET CONDITIONS BY CONTRACTOR.
- F. ALL GATES TO BE WALL BRACKETS. THE CONTRACTOR TO VERIFY GATE BRACKET TO WEIR-HIGHNESS FROM WEIR-HIGHNESS.
- G. CONTRACTOR TO PROVIDE A MINIMUM 1/2" MIN. FROM FINISH FROM FINISH. CONTRACTOR TO VERIFY WEIR-HIGHNESS FROM FINISH AND FINISH FROM FINISH.

KEYNOTES - ELEVATIONS

- 1 CONCRETE TILT-UP PANELS, FINISH: EXPOSED AGGREGATE. SEE PLAN FOR DETAILS. CONTRACTOR TO VERIFY ALL WEIR-HIGHNESS ADJUSTMENT TO ADJUSTMENT FROM WEIR-HIGHNESS.
- 2 CONCRETE TILT-UP PANELS, FINISH: EXPOSED AGGREGATE. SEE PLAN FOR DETAILS. CONTRACTOR TO VERIFY ALL WEIR-HIGHNESS ADJUSTMENT TO ADJUSTMENT FROM WEIR-HIGHNESS.
- 3 CONCRETE TILT-UP PANELS, FINISH: EXPOSED AGGREGATE. SEE PLAN FOR DETAILS. CONTRACTOR TO VERIFY ALL WEIR-HIGHNESS ADJUSTMENT TO ADJUSTMENT FROM WEIR-HIGHNESS.
- 4 CONCRETE TILT-UP PANELS, FINISH: EXPOSED AGGREGATE. SEE PLAN FOR DETAILS. CONTRACTOR TO VERIFY ALL WEIR-HIGHNESS ADJUSTMENT TO ADJUSTMENT FROM WEIR-HIGHNESS.
- 5 CONCRETE TILT-UP PANELS, FINISH: EXPOSED AGGREGATE. SEE PLAN FOR DETAILS. CONTRACTOR TO VERIFY ALL WEIR-HIGHNESS ADJUSTMENT TO ADJUSTMENT FROM WEIR-HIGHNESS.
- 6 CONCRETE TILT-UP PANELS, FINISH: EXPOSED AGGREGATE. SEE PLAN FOR DETAILS. CONTRACTOR TO VERIFY ALL WEIR-HIGHNESS ADJUSTMENT TO ADJUSTMENT FROM WEIR-HIGHNESS.
- 7 CONCRETE TILT-UP PANELS, FINISH: EXPOSED AGGREGATE. SEE PLAN FOR DETAILS. CONTRACTOR TO VERIFY ALL WEIR-HIGHNESS ADJUSTMENT TO ADJUSTMENT FROM WEIR-HIGHNESS.
- 8 CONCRETE TILT-UP PANELS, FINISH: EXPOSED AGGREGATE. SEE PLAN FOR DETAILS. CONTRACTOR TO VERIFY ALL WEIR-HIGHNESS ADJUSTMENT TO ADJUSTMENT FROM WEIR-HIGHNESS.
- 9 CONCRETE TILT-UP PANELS, FINISH: EXPOSED AGGREGATE. SEE PLAN FOR DETAILS. CONTRACTOR TO VERIFY ALL WEIR-HIGHNESS ADJUSTMENT TO ADJUSTMENT FROM WEIR-HIGHNESS.
- 10 CONCRETE TILT-UP PANELS, FINISH: EXPOSED AGGREGATE. SEE PLAN FOR DETAILS. CONTRACTOR TO VERIFY ALL WEIR-HIGHNESS ADJUSTMENT TO ADJUSTMENT FROM WEIR-HIGHNESS.

GENERAL NOTES - ELEVATIONS

- A. ALL FINISH COLOUR CHANGES TO OCCUR AT FIRST CORNER UNLESS NOTED OTHERWISE.
- B. ALL FINISH FINISH ARE TO BE FINISH UNLESS NOTED OTHERWISE.
- C. G.L.P. IS TOP OF FINISH CONCRETE.
- D. F.F.S. = FINISH FLOOR FINISH.
- E. FINISH FLOOR FINISH. CONTRACTOR TO VERIFY ALL WEIR-HIGHNESS ADJUSTMENT TO ADJUSTMENT FROM WEIR-HIGHNESS.
- F. CONTRACTOR TO PROVIDE SUFFICIENT WEIR-HIGHNESS FROM WEIR-HIGHNESS ADJUSTMENT TO ADJUSTMENT FROM WEIR-HIGHNESS.
- G. CONTRACTOR TO PROVIDE SUFFICIENT WEIR-HIGHNESS FROM WEIR-HIGHNESS ADJUSTMENT TO ADJUSTMENT FROM WEIR-HIGHNESS.
- H. CONTRACTOR TO PROVIDE SUFFICIENT WEIR-HIGHNESS FROM WEIR-HIGHNESS ADJUSTMENT TO ADJUSTMENT FROM WEIR-HIGHNESS.
- I. CONTRACTOR TO PROVIDE SUFFICIENT WEIR-HIGHNESS FROM WEIR-HIGHNESS ADJUSTMENT TO ADJUSTMENT FROM WEIR-HIGHNESS.
- J. CONTRACTOR TO PROVIDE SUFFICIENT WEIR-HIGHNESS FROM WEIR-HIGHNESS ADJUSTMENT TO ADJUSTMENT FROM WEIR-HIGHNESS.

COLOR SCHEDULE - ELEVATIONS

1	CONCRETE TILT-UP PANEL	FINISH: EXPOSED AGGREGATE
2	CONCRETE TILT-UP PANEL	FINISH: EXPOSED AGGREGATE
3	CONCRETE TILT-UP PANEL	FINISH: EXPOSED AGGREGATE
4	CONCRETE TILT-UP PANEL	FINISH: EXPOSED AGGREGATE
5	CONCRETE TILT-UP PANEL	FINISH: EXPOSED AGGREGATE
6	CONCRETE TILT-UP PANEL	FINISH: EXPOSED AGGREGATE
7	CONCRETE TILT-UP PANEL	FINISH: EXPOSED AGGREGATE
8	CONCRETE TILT-UP PANEL	FINISH: EXPOSED AGGREGATE
9	CONCRETE TILT-UP PANEL	FINISH: EXPOSED AGGREGATE
10	CONCRETE TILT-UP PANEL	FINISH: EXPOSED AGGREGATE

GLAZING LEGEND

- 1 BRONZE GLAZING
- 2 TINTED GLAZING

HPA
Hatch Professional Architects

HPA, Inc.
16301 Cardwell Avenue, - Box #100
Irvine, CA 92618
Tel: 949-452-1770
Fax: 949-452-0891
Email: hpa@hpaarch.com



XXXXX
XXXXXX
XXXXXX

Project:
REV WHEELS BUSINESS PARK

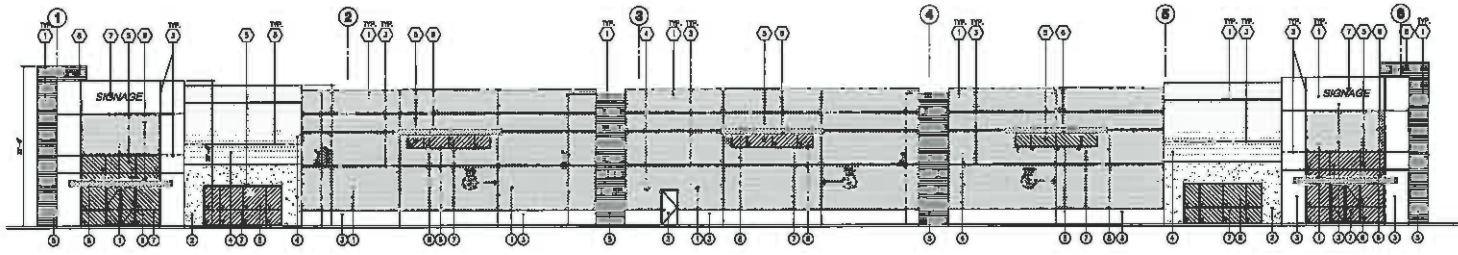
Irvine, CA

Consultants:
 CIVIL: SOH & ASSOC.
 STRUCTURAL: KRANER
 MECHANICAL: AGIS
 PLUMBING: JOHNSON
 ELECTRICAL: TBL
 LANDSCAPE: HUNTER
 FIRE PROTECTION: CARON
 SOLSENBERRY: NORCAL

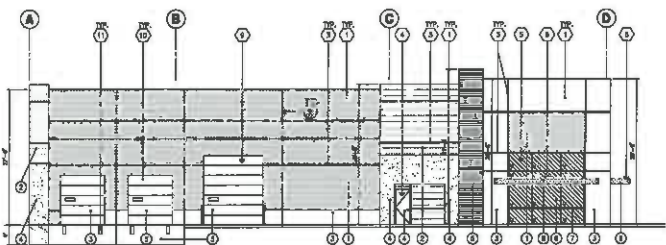
Title: ELEVATION

Project Number: 16159
 Drawn by: CC
 Date: 08/05/16

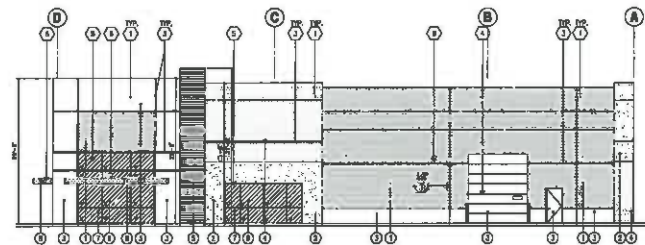
Sheet:
A3.1



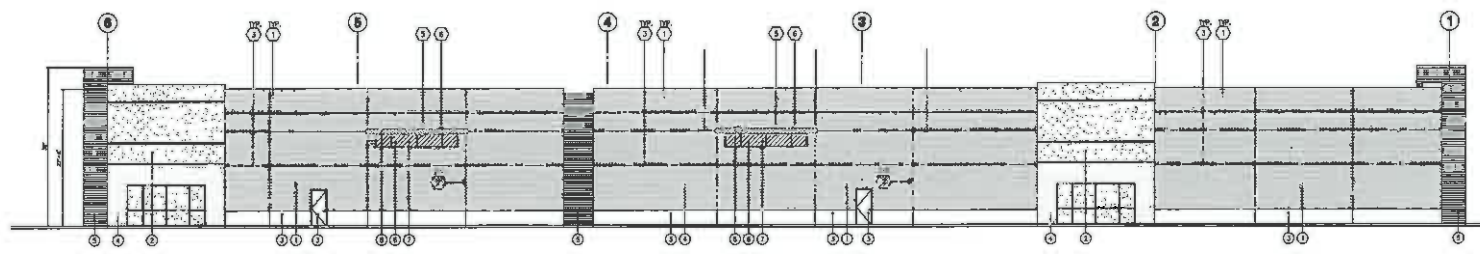
BUILDING 2 & 3 - EAST ELEVATION
SCALE: 1/8" = 1'-0" (A)



BUILDING 2 & 3 - SOUTH ELEVATION
SCALE: 1/8" = 1'-0" (B)



BUILDING 2 & 3 - NORTH ELEVATION
SCALE: 1/8" = 1'-0" (B)



BUILDING 2 & 3 - WEST ELEVATION
SCALE: 1/8" = 1'-0" (D)

GENERAL NOTES - SCREEN WALLS

- A. CONTRACTOR TO VERIFY ALL SCREENS WITH CURRENT ONE PLAN & VERIFY ONE COMPLETELY VERIFY ALL TOP OF WALL HEIGHTS STAYED IN PROPOSED SPACES.
- B. FINISH OF ALL WALLS MUST BE 8'-0" O.C. IN SCREEN WALLS & 8'-0" O.C. ON TRUCK VERT. WALL.
- C. CONTRACTOR TO PROVIDE BASH-RESISTING SCREEN TO SCREEN WALL & ALL REMAINS OF REMAINING CONCRETE FINISH INTERFERING IN VIEW OF WALL SHALL CONTRACTOR TO REMOVE WATERPROOFING RESPONSIBILITY TO REMOVE FROM TO RECONSTRUCT.
- D. SCREEN WALLS TO BE SUBMITTED UNDER SEPARATE PERMIT FROM BASHING NEIGHBORING PER COUNTY DEPARTMENT.
- E. SITE INVESTIGATION TO PROVIDE DETAILING OF DATA TO SCREENING AND BASHING FOR CITY REGULATIONS COMPLIANCE TO AND VERIFY ALL DIMENSIONS, DETAILS AND ETC. CONFORMANCE BY CONTRACTOR.
- F. ALL GATES TO BE WALL FINISH, AND CONTRACTOR TO VERIFY SHOP DRAWINGS TO VERIFY FROM TO RECONSTRUCT.
- G. CONTRACTOR TO PROVIDE A MINIMUM 1/2" OF 1/2" WALL SCREEN WALL FROM FINISH (CONCRETE WALL) CONTRACTOR TO VERIFY SCREEN WALL HEIGHT AND FINISH IN FIELD.

KEYNOTES - ELEVATIONS

1. CONCRETE TILT-UP PANELS, FINISH SHALL BE SMOOTH, POLISHED, INTERLOCKED. INTERLOCKED WALL SHALL BE FINISHED WITH PROTECTIVE COATING AND A 1/2" OF 1/2" SAND. PROTECTIVE COATING TO BE APPLIED TO ALL SURFACES OF THE TILT-UP PANELS, NOT REQUIRED AT JOINTS AND CORNERS OR AT PANEL JOINTS.
2. SAND FINISH, ALL FINISH TO HAVE A MIN. OF 1/2" OF SAND FINISH. FINISH TO HAVE MINIMUM FINISH FROM JOINTS.
3. THE 1/2" FIN. SAND FINISH SHALL BE APPLIED TO ALL SURFACES OF THE TILT-UP PANELS, NOT REQUIRED AT JOINTS AND CORNERS OR AT PANEL JOINTS.
4. ALL SURFACES OF FINISH TO BE SMOOTH FINISH, ALL SURFACES OF FINISH TO BE SMOOTH FINISH, ALL SURFACES OF FINISH TO BE SMOOTH FINISH.
5. FINISH SHALL BE SMOOTH FINISH, ALL SURFACES OF FINISH TO BE SMOOTH FINISH, ALL SURFACES OF FINISH TO BE SMOOTH FINISH.
6. FINISH SHALL BE SMOOTH FINISH, ALL SURFACES OF FINISH TO BE SMOOTH FINISH, ALL SURFACES OF FINISH TO BE SMOOTH FINISH.
7. FINISH SHALL BE SMOOTH FINISH, ALL SURFACES OF FINISH TO BE SMOOTH FINISH, ALL SURFACES OF FINISH TO BE SMOOTH FINISH.
8. FINISH SHALL BE SMOOTH FINISH, ALL SURFACES OF FINISH TO BE SMOOTH FINISH, ALL SURFACES OF FINISH TO BE SMOOTH FINISH.
9. FINISH SHALL BE SMOOTH FINISH, ALL SURFACES OF FINISH TO BE SMOOTH FINISH, ALL SURFACES OF FINISH TO BE SMOOTH FINISH.
10. FINISH SHALL BE SMOOTH FINISH, ALL SURFACES OF FINISH TO BE SMOOTH FINISH, ALL SURFACES OF FINISH TO BE SMOOTH FINISH.
11. FINISH SHALL BE SMOOTH FINISH, ALL SURFACES OF FINISH TO BE SMOOTH FINISH, ALL SURFACES OF FINISH TO BE SMOOTH FINISH.

GENERAL NOTES - ELEVATIONS

- A. ALL PAINT COLOR CHANGES TO OCCUR AT FACE CORNER UPON WORK COMPLETION.
- B. ALL PAINT FINISHES ARE TO BE PLAIN UNLESS NOTED OTHERWISE.
- C. TYP. E.L. = TOP OF FINISH FLOOR ELEVATION.
- D. F.F. = FINISH FLOOR ELEVATION.
- E. STRUCTURAL CONSTRUCTION BRIMS, METAL ATTACHMENTS AND LATHES SHALL BE EXPOSED TO REMAIN UNPAINTED UNLESS NOTED OTHERWISE.
- F. CONTRACTOR SHALL PAINT ONE CONCRETE PANEL BY SELECTED COLOR, ADHESIVE AND VERIFY SHALL APPROVE FROM TO FINISHING RESOURCES OF BASHING.
- G. BACK SIDE OF PANELS TO HAVE SMOOTH FINISH AND BE FINISHED WITH SMOOTH FINISH.
- H. FOR FINISHING MATERIALS, ALLOW SPACE BEHIND FINISHING TO BE OPEN.
- I. FINISHING CONTRACTOR TO PROVIDE SUPPORT BRACING FROM FINISHING AND-SHIFTED INTERNAL OR BEHIND FOR A 100 YEARS SUPPLY FOR USE IN THE FINISHING OVER PERIOD OF FINISHING CONTRACTED SERVICES.

COLOR SCHEDULE - ELEVATIONS

Symbol	Material	Color
1	CONCRETE TILT-UP PANEL	PAINT FINISH SHALL BE SMOOTH FINISH
2	CONCRETE TILT-UP PANEL	PAINT FINISH SHALL BE SMOOTH FINISH
3	CONCRETE TILT-UP PANEL	PAINT FINISH SHALL BE SMOOTH FINISH
4	CONCRETE TILT-UP PANEL	PAINT FINISH SHALL BE SMOOTH FINISH
5	CONCRETE TILT-UP PANEL	PAINT FINISH SHALL BE SMOOTH FINISH
6	FINISHING MATERIAL	COLOR SHALL BE SMOOTH FINISH
7	MATERIAL	COLOR SHALL BE SMOOTH FINISH
8	GLASS	PAINT FINISH SHALL BE SMOOTH FINISH

GLAZING LEGEND

Symbol	Material
1	SPRINKLER GLASS
2	NON GLASS



HPA, Inc.
15331 Gardner Avenue, Ste. #100
Folsom, CA 95630
Tel: 916-983-1770
Fax: 916-983-0881
www.hpaconsultants.com



30000
X 0000
067000000

Project:
REV WHEELS BUSINESS PARK

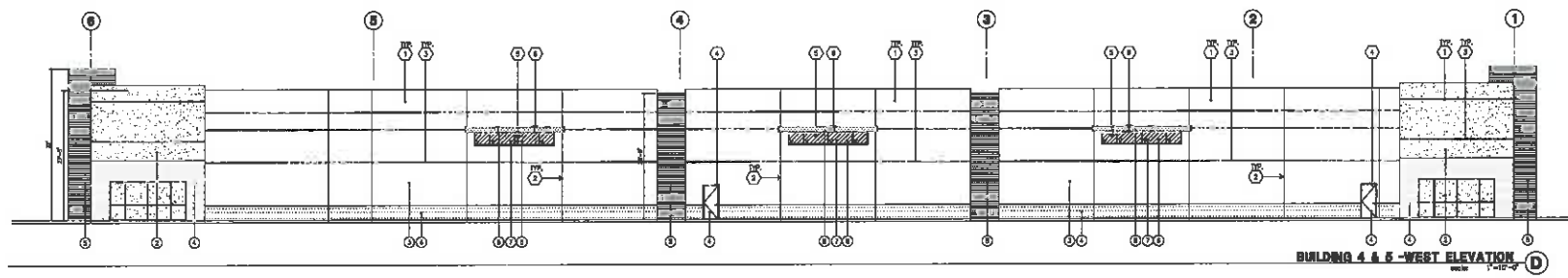
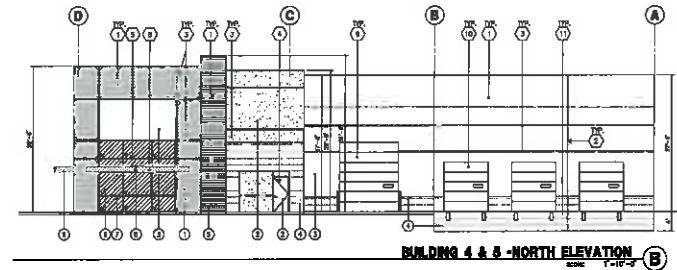
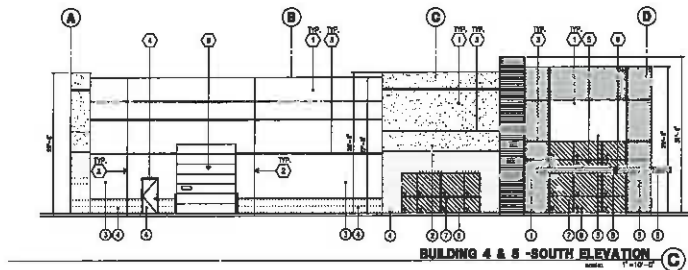
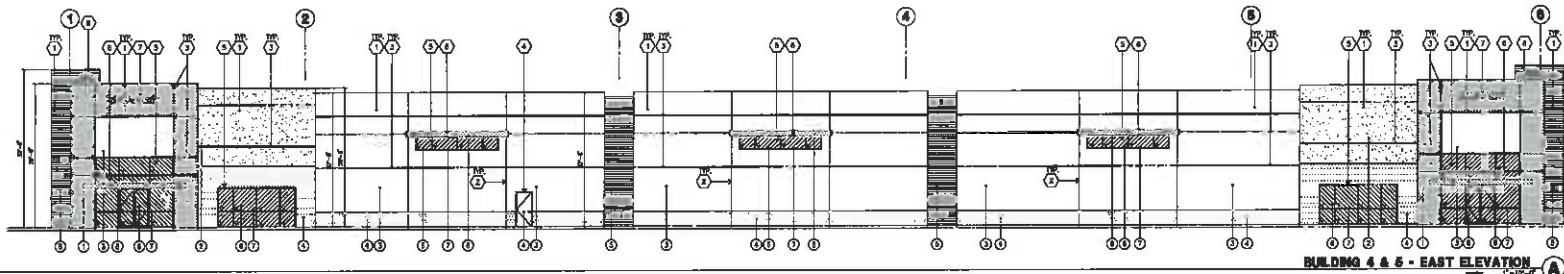
Folsom, CA

Consultants:
CIVIL STRUCTURAL MECHANICAL PLUMBING ELECTRICAL LANDSCAPE ARCHITECTURE
SOB & ASSOC. ARCHITECTS AGS JOHNSON VHL HUNTER CARROLL MORCAL

Title: ELEVATION

Project Number: 19128
Drawn by: CC
Date: 08/05/16
Revised:

Sheet
A3.2



GENERAL NOTES - SCREEN WALLS

- 1. CONTRACTOR TO VERIFY ALL SCREENS WITH CURRENT ONE-PLANE & VERIFY WITH CONTRACTOR. VERIFY ALL TOP OF WALL BEHIND CURB IS FRESH SURFACE CONCRETE.
- 2. PROVIDE 2" DIA. P.O.C. STOP WOULD IN 8'-0" O.C. AT CORNER WALLS & OTHER WALLS ON THESE WALLS.
- 3. CONTRACTOR TO PROVIDE ANCHOR-FASTENING MEMBER TO SCREEN WALLS & ALL REINFORCING & BRACING CONDITIONS. PROVIDE UNDERPINNING IN THIS SIDE OF WALL. CONCRETE TO BE PLACED IMMEDIATELY AFTER REINFORCEMENT IS IN PLACE.
- 4. SCREEN WALLS TO BE BRACKETED AGAINST EXISTING FOUNDATION FROM BUILDING EXISTING ASK CONCRETE FORMING.
- 5. SMC MANUFACTURER TO PROVIDE DETAILS OF CURB TO BE INSTALLED WITH REINFORCING AND EPP INSULATION. CONTRACTOR TO VERIFY ALL DIMENSIONS, BEAMS AND SET CONDITIONS BY CONTRACTOR.
- 6. ALL CURB TO BE BUILT BEFORE MAIN CONTRACTOR TO SUBMIT SHOP DRAWINGS TO ARCHITECT PRIOR TO INSTALLATION.
- 7. CONTRACTOR TO PROVIDE A MINIMUM 1 1/2" HIGH SCREEN WALL FROM EXISTING FOUNDATION. SCREEN WALLS TO BE CONCRETE. WALL HEIGHT AND FOOTING TO BE 18".

KEYNOTES - ELEVATIONS

- 1. CONCRETE TILT-UP PANELS/UNITED PANELS SHALL BE CAST IN PLACE. CONCRETE TO BE PLACED IN THE MIDDLE OF THE WALL. CONTRACTOR TO VERIFY AND PROVIDE WITH REINFORCING. ALL REINFORCING TO BE PLACED IN THE MIDDLE OF THE WALL. CONTRACTOR TO VERIFY AND PROVIDE WITH REINFORCING. ALL REINFORCING TO BE PLACED IN THE MIDDLE OF THE WALL. CONTRACTOR TO VERIFY AND PROVIDE WITH REINFORCING. ALL REINFORCING TO BE PLACED IN THE MIDDLE OF THE WALL.
- 2. PAINT, GYPSUM.
- 3. PAINT, GYPSUM.
- 4. PAINT, GYPSUM.
- 5. PAINT, GYPSUM.
- 6. PAINT, GYPSUM.
- 7. PAINT, GYPSUM.
- 8. PAINT, GYPSUM.
- 9. PAINT, GYPSUM.
- 10. PAINT, GYPSUM.

GENERAL NOTES - ELEVATIONS

- 1. ALL PAINT COLOR CHANGES TO BE MADE AT ABOVE CONCRETE LAYOUT NOTES.
- 2. ALL PAINT FINISHES ARE TO BE PLACED WITHIN THE FINISH.
- 3. PAINT, GYPSUM.
- 4. PAINT, GYPSUM.
- 5. PAINT, GYPSUM.
- 6. PAINT, GYPSUM.
- 7. PAINT, GYPSUM.
- 8. PAINT, GYPSUM.
- 9. PAINT, GYPSUM.
- 10. PAINT, GYPSUM.

COLOR SCHEDULE - ELEVATIONS

1	CONCRETE TILT-UP PANEL	PAINT BRICKWORK, BRICKWORK, BRICKWORK
2	CONCRETE TILT-UP PANEL	PAINT BRICKWORK, BRICKWORK, BRICKWORK
3	CONCRETE TILT-UP PANEL	PAINT BRICKWORK, BRICKWORK, BRICKWORK
4	CONCRETE TILT-UP PANEL	PAINT BRICKWORK, BRICKWORK, BRICKWORK
5	CONCRETE TILT-UP PANEL	PAINT BRICKWORK, BRICKWORK, BRICKWORK
6	CONCRETE TILT-UP PANEL	PAINT BRICKWORK, BRICKWORK, BRICKWORK
7	CONCRETE TILT-UP PANEL	PAINT BRICKWORK, BRICKWORK, BRICKWORK
8	CONCRETE TILT-UP PANEL	PAINT BRICKWORK, BRICKWORK, BRICKWORK
9	CONCRETE TILT-UP PANEL	PAINT BRICKWORK, BRICKWORK, BRICKWORK
10	CONCRETE TILT-UP PANEL	PAINT BRICKWORK, BRICKWORK, BRICKWORK

GLAZING LEGEND

1	BRICKWORK
2	PAINT BRICKWORK

HPA
ARCHITECTS

1001 N. 10th Street, Suite 100
Phoenix, AZ 85004
Tel: 602-955-1177
Fax: 602-955-0821
www.hpaarchitects.com

REV
WHEELS

3300K
K0000K
Web: 4.0000K

Project
REV WHEELS BUSINESS PARK
Phoenix, AZ

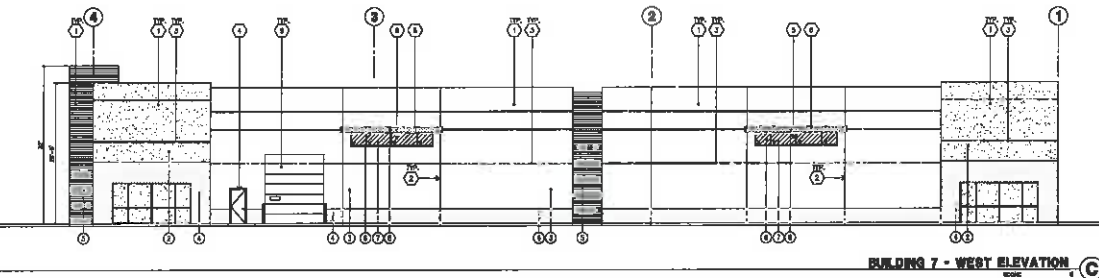
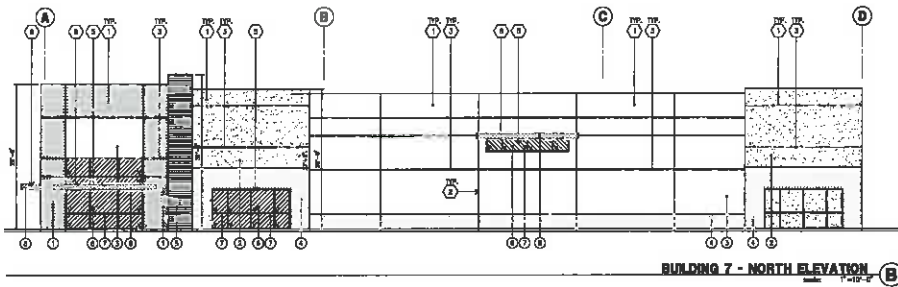
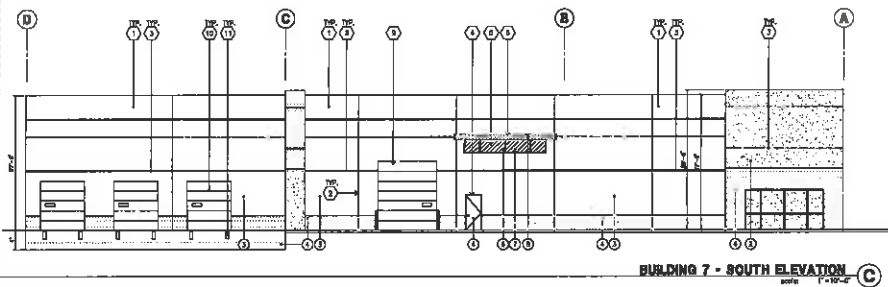
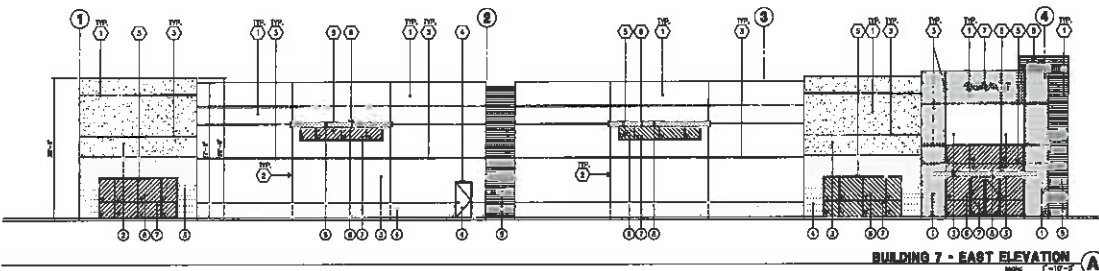
Consultants:

CIVIL STRUCTURAL MECHANICAL PLUMBING ELECTRICAL LANDSCAPE ARCHITECTURE

SEM & ASSOC. KRANER AGS JOHNSON TALL HAUNTER CARROLL MORGAL

Title: **ELEVATION**

Project Number: 16128
Drawn by: CC
Date: 08/05/2016
Revised:



GENERAL NOTES - SCREEN WALLS

- A. CONTRACTOR TO VERIFY ALL SCREEN WALL CONCRETE CURS, PLANS & VERIFY SITE CONDITIONS. VERIFY ALL TOP OF WALL ELEVATIONS IN FIELD SURFACE RECORD.
- B. PROVIDE 2" MIN. 3/4" WEEP HOLES @ 8'-0" O.C. IN SCREEN WALLS & WELLS DURING THE TRUCK WASH PROC.
- C. CONTRACTOR TO PROVIDE BOND-BREAKING MEMBER TO SCREEN WALLS IN ALL REINFORCING / BRACING CONNECTIONS. PROVIDE INTERFERENCE IN HIGH SIDE OF WALL DUE TO CONTRACTOR TO VERIFY INTERFERING OPERATIONS TO AVOID THEM TO OCCUR.
- D. SCREEN WALLS TO BE SHIMMED MAINTAIN SPACING FROM BUILDING SURFACES AND QUALITY FINISHES.
- E. GATE STRUCTURES TO PROVIDE OUTWARD OF GATES TO WITHSTAND WIND PRESSURES PER NEW REQUIREMENTS. CONTRACTOR TO VERIFY ALL DIMENSIONS, DETAILS AND SITE CONDITIONS BY CONTRACTOR.
- F. ALL WELLS TO BE WALL BRACKETS. BUT CONTRACTOR TO SUBMIT SHOP DRAWINGS TO ARCHITECT PRIOR TO INSTALLATION.
- G. CONTRACTOR TO PROVIDE A MINIMUM 14'-0" FREE SCREEN WALL FROM TRUCK WASH STREET SIDE. CONTRACTOR TO MAINTAIN SCREEN WALL HEIGHT AND FOOTING IN FIELD.

KEYNOTES - ELEVATIONS

1. CONCRETE TILT-UP PANELS/WALLS, FINISH CONC. PER SPEC. DIVISION 5. CONCRETE TILT-UP PANELS TO BE FINISHED TO FACE AND PROTECT FROM WEATHER AND SOILING. PROVIDE WEATHER-RESISTANT AND SOILING PROTECTION TO FACE OF WALLS AND FINISHES TO FACE AND FINISHES TO FACE.
2. FINISH CONC. ALL WELLS TO HAVE A MIN. OF 2" O.C. REINFORCING. PROVIDE DOWN TO WASH SURFACE TO MAINTAIN BUILDING FIELD CLEAR.
3. 2" x 2" x 1/4" WELLS WITH 1/4" DIA. 1/4" DIA. HOLES. PROVIDE TO FACE AND FINISHES TO FACE. PROVIDE TO FACE AND FINISHES TO FACE. PROVIDE TO FACE AND FINISHES TO FACE.
4. ADDITIONAL STRUCTURAL FINISHES TO FACE AND FINISHES TO FACE. PROVIDE TO FACE AND FINISHES TO FACE. PROVIDE TO FACE AND FINISHES TO FACE.
5. 1/4" x 1/4" x 1/4" WELLS WITH 1/4" DIA. 1/4" DIA. HOLES. PROVIDE TO FACE AND FINISHES TO FACE. PROVIDE TO FACE AND FINISHES TO FACE.
6. METAL CHAIRS.
7. POTENTIAL SOILING LOCATIONS.
8. STREET ADDRESS NUMBER.
9. 2" x 2" x 1/4" WELLS WITH 1/4" DIA. 1/4" DIA. HOLES. PROVIDE TO FACE AND FINISHES TO FACE. PROVIDE TO FACE AND FINISHES TO FACE.
10. 1/4" x 1/4" x 1/4" WELLS WITH 1/4" DIA. 1/4" DIA. HOLES. PROVIDE TO FACE AND FINISHES TO FACE. PROVIDE TO FACE AND FINISHES TO FACE.
11. ROCK BRACKETS. ROCK BRACKETS TO BE A MIN. OF 4" x 1/2" HIGH OF 1/2" THICKNESS. SEE REQUIREMENTS TO KEEP TRUCK FROM HITTING UPPER PANEL.

GENERAL NOTES - ELEVATIONS

- A. ALL PAINT COLORS CHANGES TO OCCUR AT ABOVE ELEVATIONS UNLESS NOTED OTHERWISE.
- B. ALL PAINT FINISHES ARE TO BE FLAT UNLESS NOTED OTHERWISE.
- C. T.O.P. O.C. TOP OF FINISHES ELEVATIONS.
- D. FIN. - FINISH FLOOR ELEVATIONS.
- E. EXISTING CONCRETE CURS WITH ATTACHMENTS AND UNITS SHALL BE REINFORCED TO MEET 80% WIND EXPOSURE "C" WIND. CONTRACTOR SHALL VERIFY SHOP DRAWINGS PRIOR TO INSTALLATION.
- F. CONTRACTOR SHALL VERIFY AND CORRECT FINISHES BY EXISTING CURS, REINFORCEMENT AND OTHER WALL APPROX. PRIOR TO FINISHING WORK OF BUILDING.
- G. ROCK SIDE OF FINISHES TO HAVE 80% WIND FINISH AND BE FINISHED WITH ELASTOMERIC POINT.
- H. FOR FINISHES GLAZING, ALLOW FINISHES BEING FINISHED TO FINISH.
- I. EXISTING CONTRACTOR TO PROVIDE SHREDDER STRIPING PAINT ANYWAYS AND ORIENTS STRIPING TO BE PAINTED FOR A 100 YEARS SERVICE FOR USE IN THE FINISHED SIDE REPAIRS OF FINISHES STRUCTURE SERVICES.

COLOR SCHEDULE - ELEVATIONS

1	CONCRETE TILT-UP PANEL	PAINT BRUSH OFF BLACKENED OR SILTY
2	CONCRETE TILT-UP PANEL	PAINT BRUSH OFF BLACKENED OR SILTY
3	CONCRETE TILT-UP PANEL	PAINT BRUSH OFF BLACKENED OR SILTY
4	CONCRETE TILT-UP PANEL	PAINT BRUSH OFF BLACKENED OR SILTY
5	CONCRETE TILT-UP PANEL	PAINT BRUSH OFF BLACKENED OR SILTY
6	REINFORCEMENT GLAZING	COLOR BLUE POLYURETHANE
7	WALLING	COLOR "SILTY" BRUSH OFF
8	CHAIRTY	PAINT BRUSH OFF BLACKENED OR SILTY

GLAZING LEGEND

- 1. SPACED GLASS
- 2. TINTED GLASS

HPA
HARDWARE PRODUCTS ASSOCIATION
Inc., Inc.
18831 Hardwood Avenue - Ste. #100
Irvine, CA 92615
Tel: 949-853-1770
Fax: 949-853-0851
www.hpa.com



JXXXX
KXXXX
MXXXX

Project:

REV WHEELS BUSINESS PARK

Rev. 06.04

Co-ordinators:

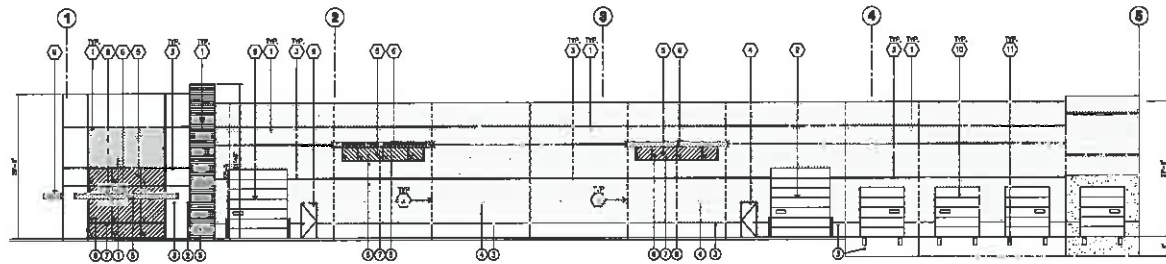
- | | |
|------------|--------------|
| CIVIL | SDM & ASSOC. |
| STRUCTURAL | KRAUSS |
| MECHANICAL | ACS |
| PLUMBING | JOHNSON |
| ELECTRICAL | TAAL |
| LANDSCAPE | HUNTER |
| INTERIOR | CARLIN |
| SEASIDE | MORCAL |

Title: ELEVATION

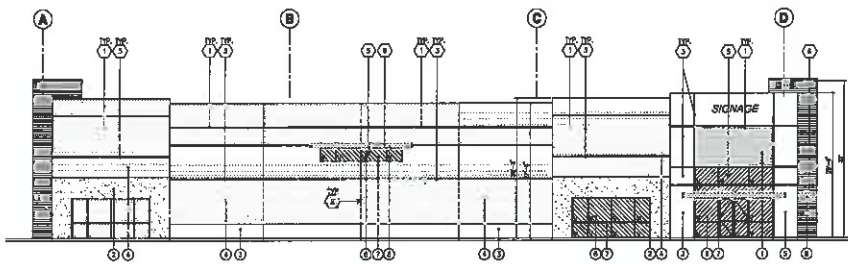
Project Number: 16189
Drawn by: CC
Date: 08/28/16
Revised:

Sheet

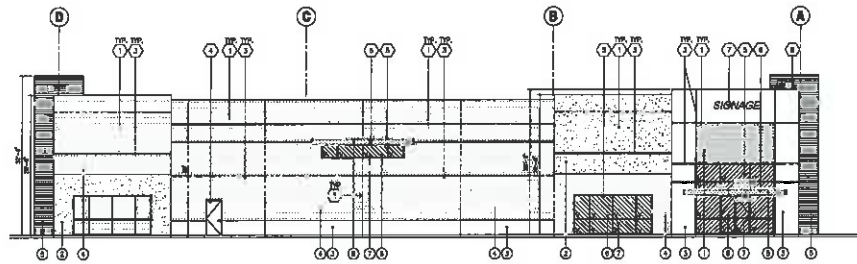
A3.5



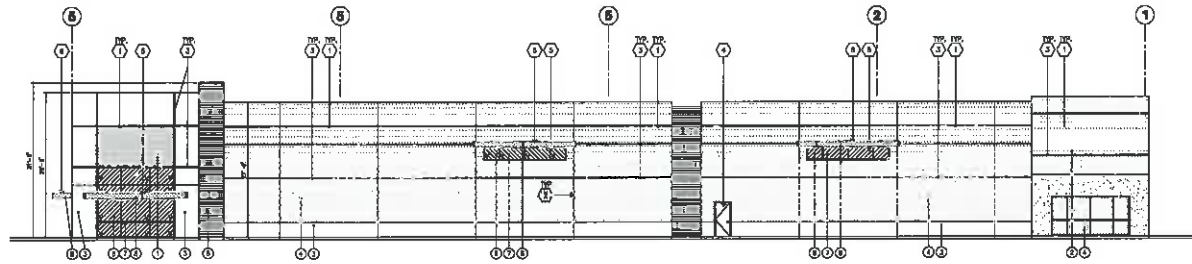
BUILDING 8 - EAST ELEVATION
 scale 1/8" = 1'-0" **A**



BUILDING 8 - SOUTH ELEVATION
 scale 1/8" = 1'-0" **C**



BUILDING 8 - NORTH ELEVATION
 scale 1/8" = 1'-0" **B**



BUILDING 8 - WEST ELEVATION
 scale 1/8" = 1'-0" **D**

GENERAL NOTES - SCREEN WALLS

1. CONTRACTOR TO VERIFY ALL SCREENS WITH CURRENT GAS PLUMB & VERIFY AIR COMPARTMENT. VERIFY ALL TOP OF WALL HEIGHTS (TOAS) & FINISH SURFACE HEIGHTS.
2. FINISH OF 2x4 PAULS, KEEP HOLES 8'-0" O.C. & SPACED WALLS 8' SPACED EQUAL TO TRACK SPACING.
3. CONTRACTOR TO PROVIDE WATER-PROOFING MEMBRANE TO SCREEN WALL & ALL INTERSECTIONS / JOINTS. CONTRACTOR PROVIDE WEATHERSTRIP IN HIGH SIDE OF WALL. CONTRACTOR TO NOTIFY SUBMITTALS REVISIONS TO ARCHITECT PRIOR TO INSTALLATION.
4. SCREEN WALLS TO BE INSTALLED IMMEDIATELY FOLLOWING FROM WALLING DEPARTMENT FOR COUNTY SEALS.
5. ONLY WEATHERSTRIP TO PROVIDE DRAINAGE OF WATER TO EXTERIOR. THIS WEATHERSTRIP TO BE INSTALLED TO EXTERIOR OF WALL. CONTRACTOR TO NOTIFY SUBMITTALS REVISIONS TO ARCHITECT PRIOR TO INSTALLATION.
6. ALL SCREENS TO BE FULL FINISH. THE CONTRACTOR TO MAKE SURE SCREENS TO ADHERE TO INSTALLATION.
7. CONTRACTOR TO PROVIDE A MINIMUM 1/4" OF FINISH SCREEN WALL FROM FINISH GROUND LEVEL. CONTRACTOR TO MAKE SURE WALL, FINISH AND FOOTING IS PAID.

KEYNOTES - ELEVATIONS

1. CONCRETE TILT-UP PANELS/FRAMES SHALL BE CAST WITH CONCRETE. WEATHERSTRIP ALL WALLS FROM CORNER TO CORNER AND INTERSECTIONS TO MATCH THE WALL. WEATHERSTRIP TO BE PROVIDED TO MATCH THE WALL. CONTRACTOR TO PROVIDE FINISH SURFACE TO MATCH THE WALL. CONTRACTOR TO PROVIDE FINISH SURFACE TO MATCH THE WALL.
2. FINISH JOINT.
3. FINISH JOINT. ALL WALLS TO HAVE A MIN. OF 2" OF FINISH SURFACE TO MATCH THE WALL. CONTRACTOR TO PROVIDE FINISH SURFACE TO MATCH THE WALL.
4. FINISH JOINT. ALL WALLS TO HAVE A MIN. OF 2" OF FINISH SURFACE TO MATCH THE WALL. CONTRACTOR TO PROVIDE FINISH SURFACE TO MATCH THE WALL.
5. FINISH JOINT. ALL WALLS TO HAVE A MIN. OF 2" OF FINISH SURFACE TO MATCH THE WALL. CONTRACTOR TO PROVIDE FINISH SURFACE TO MATCH THE WALL.
6. FINISH JOINT. ALL WALLS TO HAVE A MIN. OF 2" OF FINISH SURFACE TO MATCH THE WALL. CONTRACTOR TO PROVIDE FINISH SURFACE TO MATCH THE WALL.
7. FINISH JOINT. ALL WALLS TO HAVE A MIN. OF 2" OF FINISH SURFACE TO MATCH THE WALL. CONTRACTOR TO PROVIDE FINISH SURFACE TO MATCH THE WALL.
8. FINISH JOINT. ALL WALLS TO HAVE A MIN. OF 2" OF FINISH SURFACE TO MATCH THE WALL. CONTRACTOR TO PROVIDE FINISH SURFACE TO MATCH THE WALL.
9. FINISH JOINT. ALL WALLS TO HAVE A MIN. OF 2" OF FINISH SURFACE TO MATCH THE WALL. CONTRACTOR TO PROVIDE FINISH SURFACE TO MATCH THE WALL.
10. FINISH JOINT. ALL WALLS TO HAVE A MIN. OF 2" OF FINISH SURFACE TO MATCH THE WALL. CONTRACTOR TO PROVIDE FINISH SURFACE TO MATCH THE WALL.

GENERAL NOTES - ELEVATIONS

1. ALL PAINT COLOR CHANGES TO BE MADE BY OWNER. CONTRACTOR TO VERIFY COLOR CHANGES TO BE MADE BY OWNER. CONTRACTOR TO VERIFY COLOR CHANGES TO BE MADE BY OWNER.
2. ALL PAINT PROJECT ARE TO BE PAINT MATCHES NOTED OTHERWISE.
3. T&E: 1/4" TOP OF FINISH SURFACE.
4. T&E: 1/4" TOP OF FINISH SURFACE.
5. T&E: 1/4" TOP OF FINISH SURFACE.
6. T&E: 1/4" TOP OF FINISH SURFACE.
7. T&E: 1/4" TOP OF FINISH SURFACE.
8. T&E: 1/4" TOP OF FINISH SURFACE.
9. T&E: 1/4" TOP OF FINISH SURFACE.
10. T&E: 1/4" TOP OF FINISH SURFACE.

COLOR SCHEDULE - ELEVATIONS

CONCRETE TILT-UP PANEL	PAINT FINISH SURFACE
1	PAINT FINISH SURFACE
2	PAINT FINISH SURFACE
3	PAINT FINISH SURFACE
4	PAINT FINISH SURFACE
5	PAINT FINISH SURFACE
6	PAINT FINISH SURFACE
7	PAINT FINISH SURFACE
8	PAINT FINISH SURFACE
9	PAINT FINISH SURFACE
10	PAINT FINISH SURFACE

GLAZING LEGEND

- 1. SPANDREL GLASS
- 2. WIND GLASS



HPA, Inc.
 18221 BARDEN AVENUE, - SUITE #100
 FORT COCKER
 162 941-885-1770
 162 941-885-0821
 email: hpa@hpa.com



2000X
 2000X
 162 941-885-0821

Project:
REV WHEELS BUSINESS PARK

Riverside, CA

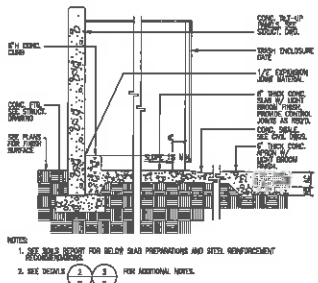
Consultants:
 CIVIL STRUCTURAL MECHANICAL ELECTRICAL LANDSCAPE ARCHITECTURE
 SOH & ASSOC. KRAMER AGS JOHNSON H&L HUNTER CARBON NORCAL

Title: **ELEVATION**

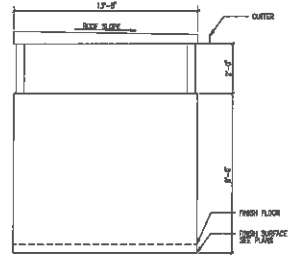
Project Number: 16169
 Drawn by: CC
 Date: 06/05/2016

Fieldwork:

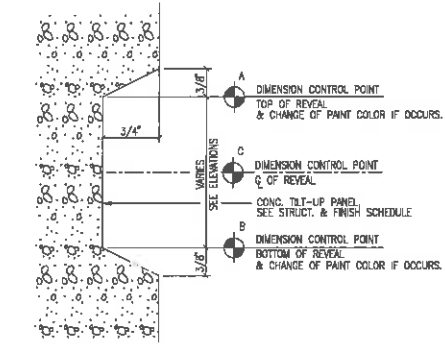
Sheet:
A3.6



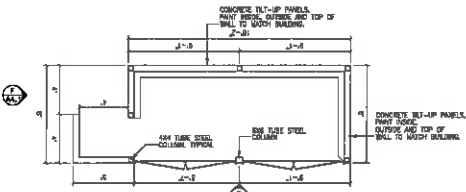
TRASH ENCLOSURE SECTION
DATE: 1/15/16
SHEET: 1/15



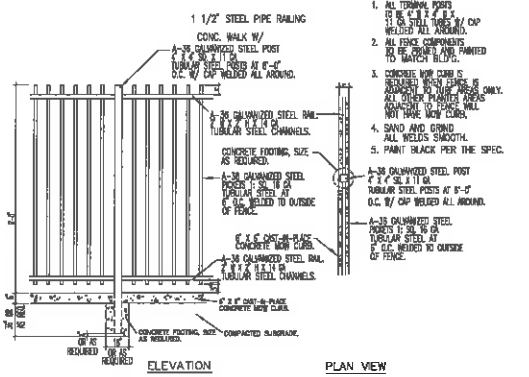
TRASH ENCLOSURE GATE ELEVATION
DATE: 1/15/16
SHEET: 1/15



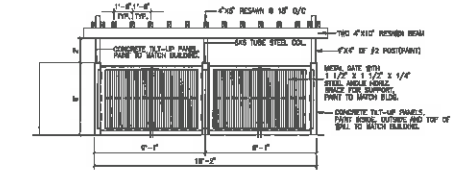
TYP. CONCRETE REVEAL
DATE: 1/15/16
SHEET: 1/15



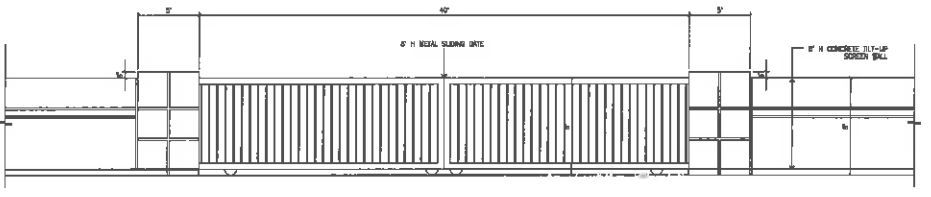
TRASH ENCLOSURE PLAN
DATE: 1/15/16
SHEET: 1/15



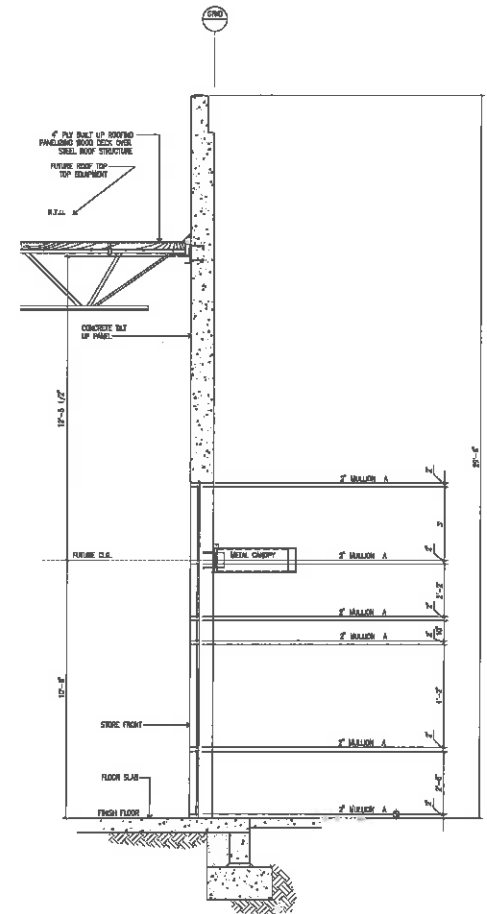
METAL FENCE DETAILS
DATE: 1/15/16
SHEET: 1/15



TRASH ENCLOSURE GATE ELEVATION
DATE: 1/15/16
SHEET: 1/15



ENLARGED SCREENWALL AND GATE ELEVATION
DATE: 1/15/16
SHEET: 1/15



WALL SECTION @ ENTRY
DATE: 1/15/16
SHEET: 1/15

HPA
INCORPORATED

Logo, Inc.
18821 bardonia avenue, -ste. #100
Piquette, MI 48672
Tel: 248-363-1770
Fax: 248-363-0461
email: logo@hpainc.com



XXXX
XXXXXX
XXXXXX

Project:
REV WHEELS BUSINESS PARK
Plymouth, CA

Consultants:
CIVIL: KHANBEKIAN ASSOC.
STRUCTURAL: AGS
MECHANICAL: JOHNSON
ELECTRICAL: VTL
LANDSCAPE: HUNTER
FIRE PROTECTION: CARSON
SOLSEBERGER NORCAL

Title: section

Project Number: 16129
Drawn by: CC
Date: 06/05/2016
Revision:

Sheet:

A4.1



HFA, Inc.
10511 Business Avenue, - Ste. #100
Plym, CA
95019
Tel: 949-883-1770
Fax: 949-883-0851
email: hfa@hfa.com

Owner:

Project:
Old 215 Frontage
Business Park

Plym, CA

Consultants:



Title:

Project Number:
Drawn by:
Date:
Revision:

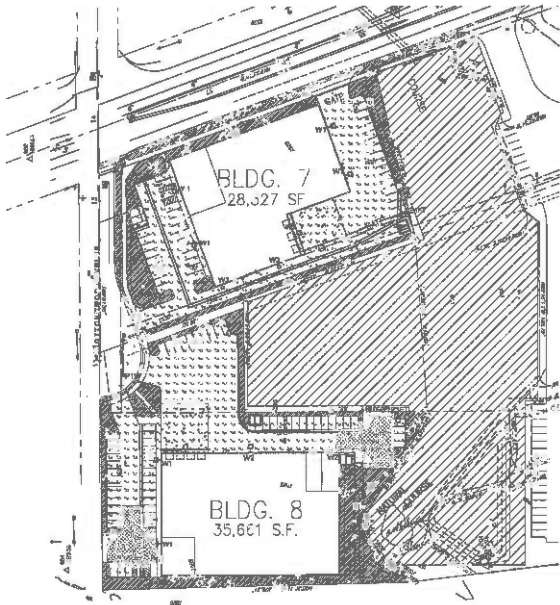
Sheet
OVERALL PHOTOMETRIC STUDY
SITING LIGHTING PLAN

E0.0

STATISTICS						
Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
Zone	+	3.3 fc	17.7 fc	0.9 fc	19.4:1	4.8:1
Zone 1	+	3.8 fc	24.9 fc	0.4 fc	62.3:1	9.8:1
Zone 2	+	3.7 fc	35.9 fc	0.9 fc	39.8:1	4.8:1
Zone 4	+	4.1 fc	17.7 fc	0.9 fc	22.3:1	5.1:1
Zone 5	+	9.4 fc	34.5 fc	0.4 fc	87.0:1	23.5:1

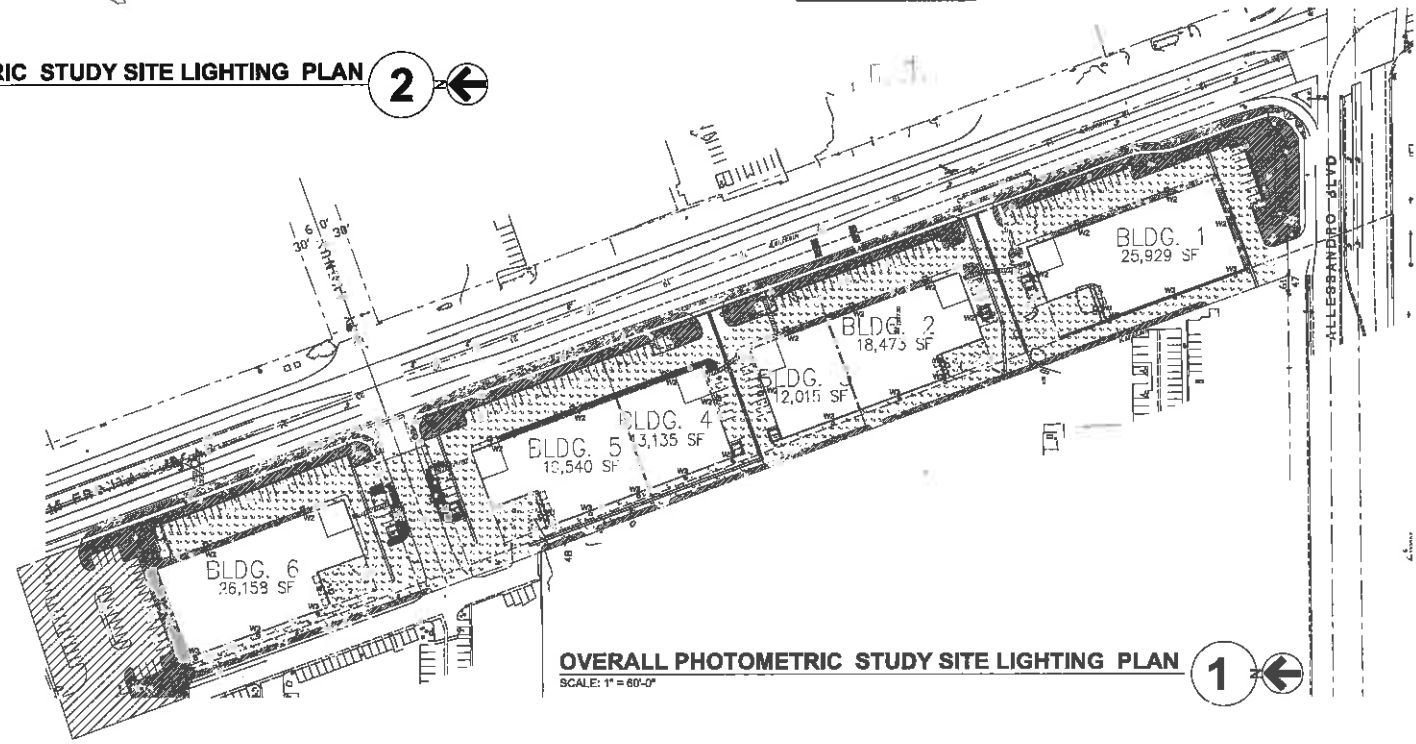
LUMINAIRE SCHEDULE									
Symbol	Label	Qty	Catalog Number	Description	Lamp	Fie	Lumens	L.F.	Watts
□	P1PT	2	CT24-FT-400- P5M05-F	CITATION	4-400W P5M0R CLEAR HOR	CT24-FT-400- P5M05-F.lam	40000	1.00	452
□	P2W	2	CT24-3-400- P5M05-F	CITATION	4-400W P5M0R CLEAR HOR	CT24-3-400- P5M05-F.lam	40000	1.00	904
□	W1	12	CT24-FT-400- P5M05-F	CITATION	4-400W P5M0R CLEAR HOR	CT24-FT-400- P5M05-F.lam	43000	1.00	452
□	W2	17	CT24-3-400- P5M05-F	CITATION	4-400W P5M0R CLEAR HOR	CT24-3-400- P5M05-F.lam	43000	1.00	452
□	W3	15	CHWB-3-100AM-F	CHALLENGER		CHWB-3-100- M167.lam	6000	1.00	120
□	P1W	2	CT24-3-400- P5M05-F-HS5	CITATION	4-400W P5M0R CLEAR HOR	CT24-3-400- P5M05-F- HS5.lam	40000	1.00	452
□	P2PT	1	CT24-FT-400- P5M05-F	CITATION	4-200W P5M0 CLEAR U	CT24-FT-400- P5M05-F.lam	22000	1.00	291

46 MILES TO PALOMAR.
METAL HALIDE IS APPROVED.



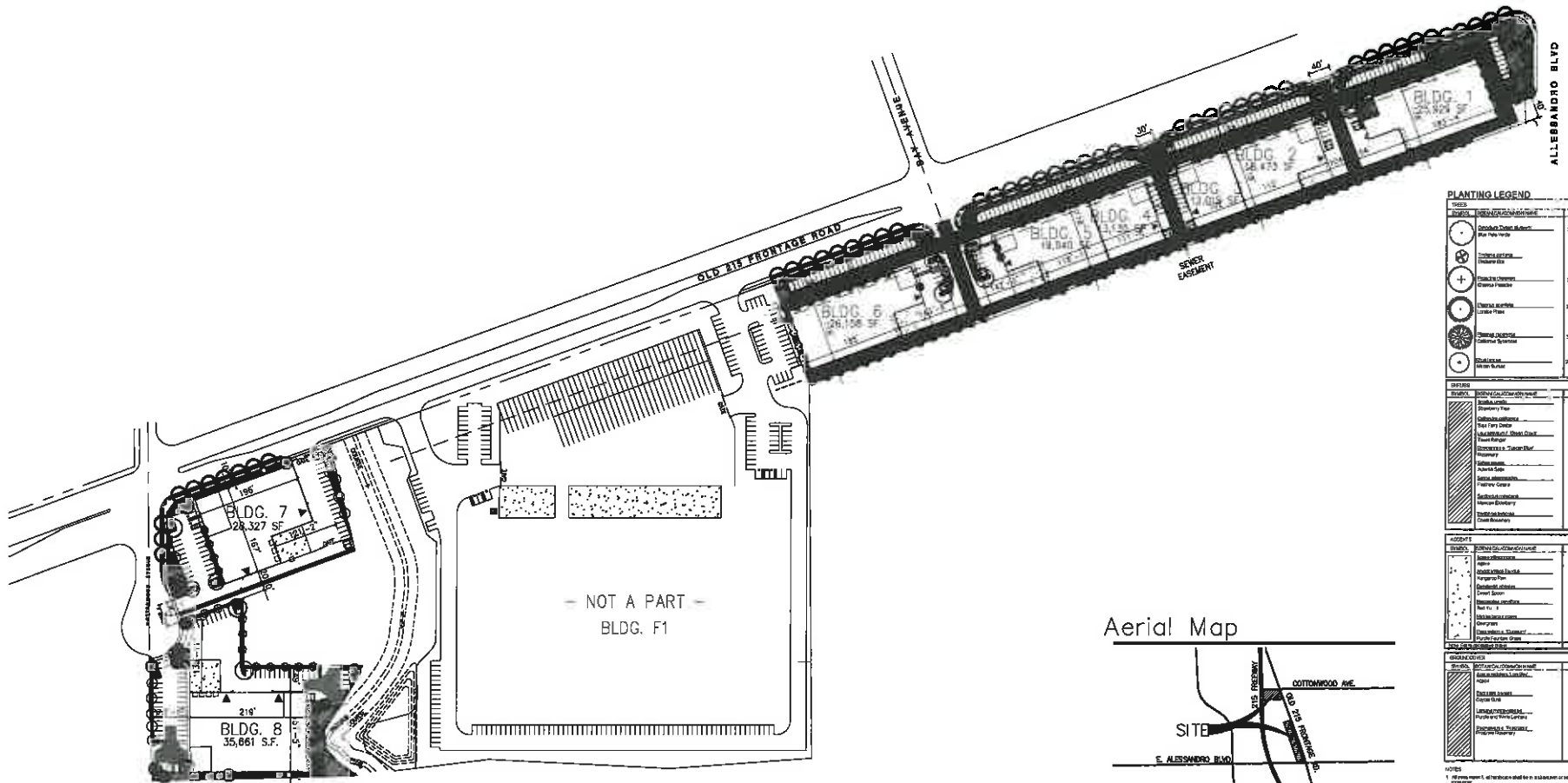
OVERALL PHOTOMETRIC STUDY SITE LIGHTING PLAN 2 ←

SCALE: 1" = 60'-0"

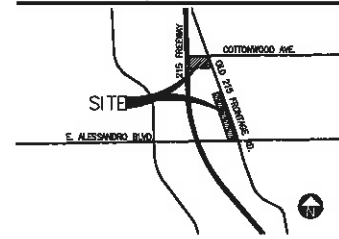


OVERALL PHOTOMETRIC STUDY SITE LIGHTING PLAN 1 ←

SCALE: 1" = 60'-0"



Aerial Map



PLANTING LEGEND

SYMBOL	DESCRIPTION	SIZE	SPACING
(Symbol)	Japanese Cedar Shrub	1 1/2" L	13'
(Symbol)	Blue Palm Tree	10.00"	11'
(Symbol)	Flowering Quince	30" Dia.	9'
(Symbol)	Flowering Quince	30" Dia.	13'
(Symbol)	Flowering Quince	30" Dia.	14'
(Symbol)	Flowering Quince	30" Dia.	15'
SPACES			
(Symbol)	Grass	5.00'	5.00'
(Symbol)	Grass	5.00'	5.00'
(Symbol)	Grass	5.00'	5.00'
(Symbol)	Grass	5.00'	5.00'
(Symbol)	Grass	5.00'	5.00'
(Symbol)	Grass	5.00'	5.00'
(Symbol)	Grass	5.00'	5.00'
(Symbol)	Grass	5.00'	5.00'
(Symbol)	Grass	5.00'	5.00'
(Symbol)	Grass	5.00'	5.00'
ADJUST			
(Symbol)	Grass	5.00'	5.00'
(Symbol)	Grass	5.00'	5.00'
(Symbol)	Grass	5.00'	5.00'
(Symbol)	Grass	5.00'	5.00'
(Symbol)	Grass	5.00'	5.00'
(Symbol)	Grass	5.00'	5.00'
PROPOSED			
(Symbol)	Grass	10.00'	5' O.C.
(Symbol)	Grass	10.00'	5' O.C.
(Symbol)	Grass	10.00'	5' O.C.
(Symbol)	Grass	10.00'	5' O.C.

NOTES

1. All areas shown, all vegetation shall be in existence on date of issue.
2. Dimensions to center of tree or shrub unless otherwise noted.
3. All trees to be planted within 24 months of date of issue.

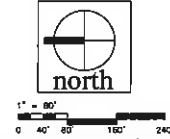
NOTE

1. The Owner is to provide all utilities to the site and to the project.
2. The Owner is to provide all utilities to the project.
3. The Owner is to provide all utilities to the project.
4. The Owner is to provide all utilities to the project.
5. The Owner is to provide all utilities to the project.

Conceptual Landscape Plan
Old 215 Frontage Business Park

Riverside, CA

September 30, 2016 / Job #15-000
 Scheme 2 - Phase II & III



**NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Thursday from 8:00 a.m. to 5:00 p.m., except Thursday November 24 (Thanksgiving Day) and by prescheduled appointment on Friday, December 2 from 8:30 a.m. to 5:00 p.m. Please send any written comments to this address.

ATTENTION: ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. The City of Riverside may hold hearings on this project and should be contacted on non-ALUC issues.

PLACE OF HEARING: Eastern Municipal Water District
2270 Trumble Road
Perris, California
[Hearing venue only—do not send written comments to this address]

DATE OF HEARING: December 8, 2016

TIME OF HEARING: 9:00 A.M.

CASE DESCRIPTION:

ZAP1220MA16 – Rev Wheel LLC (Representative: SDH & Associates, Inc.) – City of Riverside Planning Case Nos. P15-1035, P16-0556 and P16-0557. A proposal to develop eight industrial buildings (predominately warehouse) ranging from 12,015 to 35,661 square feet in gross floor area, with a cumulative total of 179,710 square feet on 10.4 acres located westerly of Old 215 Frontage Road, northerly of Alessandro Boulevard, southerly of Cottonwood Avenue, and easterly of Interstate 215, and to divide the property into eight lots so that each building will be located on a separate lot. The applicant is also proposing to amend the Sycamore Canyon Business Park Specific Plan land use designation from Retail Business Office and Industrial Support to Industrial, and establish development standards for these lots (Airport Compatibility Zone B1-APZ II of the March Air Reserve Base/Inland Port Airport Influence Area).

FURTHER INFORMATION: Contact Paul Rull at (951) 955-6893. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Mr. Sean Kelleher of the City of Riverside Planning Division at (951) 826-5712.

APPLICATION FOR MAJOR LAND USE ACTION REVIEW
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

ALUC Identification No.

ZAP1220 MA16

PROJECT PROPONENT (TO BE COMPLETED BY APPLICANT)

Date of Application 10/3/16
 Property Owner REV WHEEL, LLC Phone Number (951) 288-8544
 Mailing Address 421 MAIN ST
RIVERSIDE, CA 92501

Agent (if any) SDHE ASSOCIATES, INC. Phone Number (951) 683-3691
 Mailing Address 5225 CANYON CREST DR. STE. 71-439
RIVERSIDE, CA 92507

PROJECT LOCATION (TO BE COMPLETED BY APPLICANT)

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address (NONE ASSIGNED) W/ LY SIDE OF OLD 216 FRONTAGE ROAD
BETWEEN ALESSANDRO AND CUTTONWOOD
 Assessor's Parcel No. 263-091-015, 263-100-021 Parcel Size 456,290 S.F. (10.4 AC)
 Subdivision Name _____ Zoning Classification BMP
 Lot Number _____

PROJECT DESCRIPTION (TO BE COMPLETED BY APPLICANT)

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use VACANT
 (describe) _____
 Proposed Land Use EIGHT INDUSTRIAL BUILDINGS RANGING IN SIZE FROM 12,015
TO 40,104 SQUARE FEET. TOTAL BUILDING SQUARE FOOTAGE IS
169,943.
 (describe) _____

For Residential Uses Number of Parcels or Units on Site (exclude secondary units) _____
 For Other Land Uses Hours of Use 24 HRS./DAY
 (See Appendix C) Number of People on Site Maximum Number 253
 Method of Calculation TOTAL PARKING SPACES PROVIDED

Height Data Height above Ground or Tallest Object (including antennas and trees) 30'
 Highest Elevation (above sea level) of Any Object or Terrain on Site 1539.70 FF (BLDG. 1&2)

Flight Hazards Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight? Yes No
 If yes, describe _____

Much
 01 APZ II

REFERRING AGENCY (APPLICANT OR JURISDICTION TO COMPLETE)	
Date Received	8/22/16
Agency Name	CITY OF RIVERSIDE
Staff Contact	SEAN KELLEHER
Phone Number	(951) 826-5712
Agency's Project No.	P15-1035 P16-0556, P16-0557
Type of Project	<input type="checkbox"/> General Plan Amendment <input type="checkbox"/> Zoning Amendment or Variance <input checked="" type="checkbox"/> Subdivision Approval PARCEL MAP <input type="checkbox"/> Use Permit <input type="checkbox"/> Public Facility <input checked="" type="checkbox"/> Other SPA & DR

A. **NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

B. **SUBMISSION PACKAGE:**

ALUC REVIEW

- 1 Completed Application Form
- 1 Project Site Plan – Folded (8-1/2 x 14 max.)
- 1 Elevations of Buildings - Folded
- 1 Each . 8 ½ x 11 reduced copy of the above
- 1 8 ½ x 11 reduced copy showing project in relationship to airport.
- 1 Set . Floor plans for non-residential projects
- 4 Sets. . Gummed address labels of the Owner and representative (*See Proponent*).
- 1 Set . Gummed address labels of all property owners within a 300' radius of the project site. If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10), with ALUC return address.
- 4 Sets. . Gummed address labels of the referring agency (City or County).
- 1 Check for Fee (See Item "C" below)

STAFF REVIEW (Consult with ALUC staff planner as to whether project qualifies)

- 1 Completed Application Form
- 1 Project Site Plans – Folded (8-1/2 x 14 max.)
- 1 Elevations of Buildings - Folded
- 1 8 ½ x 11 Vicinity Map
- 1 Set . Gummed address labels of the Owner and representative (*See Proponent*).
- 1 Set . Gummed address labels of the referring agency.
- 1 Check for review—See Below

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 4.1

HEARING DATE: December 8, 2016

CASE SUMMARY:

CASE NUMBER: ZAPEA02HR13 – Airport Land Use Commission

LEAD AGENCY: Riverside County Airport Land Use Commission
("Commission")

JURISDICTION CASE NO: Not Applicable

MAJOR ISSUES: The City of Hemet has expressed concerns regarding the prohibition on intermediate residential densities within Compatibility Zone D. The Plan allows for densities of 4.5 to 5.0 dwelling units per acre and higher, but the City would like the Plan to allow densities as low as 3.0 dwelling units per acre in Zone D. The Countywide Policies also require that projects at least 10 acres in size incorporate minimum percentages of open area (20% in Zone C and 10% in Zone D). The City objects to this requirement and requests that the Commission waive project-specific open area requirements and instead accept a list of open areas in the general vicinity. A commercial land owner has noted that the extent of Compatibility Zones B1 and C northeasterly of the runway exceeds California Airport Land Use Planning Handbook guidelines for comparable safety zones and that the proposed intensity allowances are more restrictive than those allowed in the Handbook. It should be noted in this regard that Hemet-Ryan Airport is also home to a Cal Fire air attack base flying aircraft larger than other general aviation aircraft. The commercial landowner also suggests that Hemet-Ryan should be considered an urban airport, rather than a suburban airport, in light of the existing intensities and traffic levels in the area.

RECOMMENDATION: Staff recommends that the Commission open the public hearing, consider testimony, and, after the close of public hearing, that the Commission:

- (1) Adopt the Negative Declaration for the Hemet-Ryan Airport Land Use Compatibility Plan, and thereby find that:**
 - a. Having considered the Initial Study/Negative Declaration, the comments received during the public review process, and the record before the Commission, there is no substantial evidence that adoption of the Hemet-Ryan Airport Land Use Compatibility Plan would have a significant effect on the environment; and**

b. The Initial Study/Negative Declaration reflects the Commission's independent judgment and analysis;

(2) Adopt the Hemet-Ryan Airport Land Use Compatibility Plan; and

(3) Adopt Resolution No. 2016-03 memorializing the Commission's actions.

PROJECT DESCRIPTION:

The proposed project is the Commission's adoption of the 2016 Hemet-Ryan Airport Land Use Compatibility Plan ("Hemet ALUCP"), which includes "Additional Compatibility Policies" specifically tailored to the land use environs of Hemet-Ryan Airport and an Airport Influence Area ("AIA") with new boundaries. The new AIA includes the area in which noise, safety, airspace protection, or overflight concerns may significantly affect land uses or necessitate restrictions on those uses, as determined by the Commission. Accordingly, the Compatibility Plan includes policies for determining whether a proposed development project, lying within the AIA, is consistent with the Compatibility Plan and the objectives set forth in the State Aeronautics Act, which include ensuring the continued operation of public-use airports (such as Hemet-Ryan Airport), while simultaneously protecting the public's health, safety, and welfare. (See Pub. Util. Code, §§21670-21679.5.)

The Commission is required by state law to prepare airport land use compatibility plans for the airport influence areas around public-use airports. Airport officials project that activity levels at Hemet-Ryan Airport will continue and likely increase over time.

Much of the portion of the City of Hemet located westerly of downtown Hemet, as well as easterly areas of Winchester and Green Acres, are in the vicinity of the Airport and are affected by aircraft noise and overflight. The Commission's charge is to protect the public from excessive noise and safety hazards. Therefore, the Hemet-Ryan ALUCP imposes limits on the density and intensity of future land use development in the AIA.

PROJECT LOCATION:

Hemet-Ryan Airport is a County-owned, public-use airport located southerly of Acacia and Florida Avenues, easterly of Warren Road, northerly of Stetson Avenue, and westerly of Sanderson Avenue in the City of Hemet.

The proposed boundaries of the AIA are defined by an arc consisting of a set of points located 14,200 feet from the runway ends depicted on the Airport Layout Plan dated May 2013, except to the north where it does not extend northerly of Esplanade Avenue, the boundary line between the cities of Hemet and San Jacinto. The Airport Layout Plan identifies – as a future improvement to Hemet-Ryan Airport – a 500-foot easterly extension of Runway 5-23, with use of declared distances. All boundaries of

Compatibility Zones A through D, however, are measured from the physical ends of the existing runways.

The area that would be subjected to development restrictions identified in the proposed Hemet ALUCP, and the corresponding potential for displaced development, would be smaller than the area described above (i.e., not all of the AIA is subject to the potential for displaced development). The potentially affected area is limited to areas within 6,000 feet of the runway centerline and its straight-line northeasterly and southwesterly extensions.

BACKGROUND:

In 2004, the Commission prepared and adopted new Countywide Policies and new Airport Land Use Compatibility Plans ("ALUCPs") for several airports within the County. These ALUCPs are available online at www.rcaluc.org. The new ALUCPs were prepared pursuant to the 2002 edition of the *California Airport Land Use Planning Handbook*, published by the State of California Department of Transportation, Division of Aeronautics. This *Handbook* is the guidebook for land use planning in AIAs throughout the State of California. The 2004 ALUCPs replaced the previously adopted Comprehensive Land Use Plans ("CLUPs"), which were prepared in the 1990s prior to the 2002 version of the *Handbook*.

The Countywide Policies adopted in 2004 have not been applicable to the Hemet-Ryan Airport Influence Area (AIA), which to date is subject to the 1992 Hemet-Ryan Airport Comprehensive Airport Land Use Plan ("HRACALUP"). The HRACALUP divided the AIA into four areas, labeled Areas I, II, and III, with a Transition Area consisting of the outer portion of Area II and the inner portion of Area III. Residential densities were limited to a maximum of one dwelling unit per 2½ acres in Areas I and II (except for those portions of Area II in the Transition Area). Places of assembly (including restaurants) are also prohibited in Area II (other than in the Transition Area).

The Hemet ALUCP would impose new restrictions on development. In some areas, a land use pattern based upon the proposed project may result in a reduced allowable residential density or a restriction in density range to either higher or lower densities than permitted by the adopted City of Hemet General Plan.

AIRPORT LAND USE COMPATIBILITY ZONES:

The Hemet ALUCP delineates six (6) airport land use compatibility zones ("Compatibility Zones"): A, B1, B2, C, D, and E. Safety hazards and noise are greatest in Compatibility Zone A, and decline as one moves farther from the runway and its extended centerline. Each zone has criteria limiting densities and intensities; however, Compatibility Zone E, located farthest from the runway and primary flight paths, has no residential density or land use intensity restrictions (other than restrictions on large assemblages of people and prohibition of uses that are hazards to flight).

Compatibility Zone A: Compatibility Zone A is the Runway Protection Zone, prohibiting all structures except those with locations set by their aeronautical function, assemblages of people,

objects exceeding FAR Part 77 height limits, storage of hazardous materials, and hazards to flight. These restrictions are easily linked to safety concerns and Federal Aviation Regulations.

In 2011, the Department of Transportation, Division of Aeronautics issued a new edition of the *California Airport Land Use Planning Handbook* that distinguishes between suburban and rural airports in regard to recommended intensity limits in the various safety zones. Accordingly, the Hemet ALUCP incorporates an Additional Compatibility Policy that allows for more generous limits than those applied to lands within the Airport Influence Areas surrounding the airports subject to the Compatibility Plans adopted in 2004.

Compatibility Zone B1: Compatibility Zone B1 is the Inner Approach/Departure Zone. New residential development in Compatibility Zone B1 is limited to 0.05 dwelling units per acre – an average density of one dwelling unit per twenty (20) acres. Non-residential development may maintain a maximum intensity of forty (40) persons per acre (averaged over a site), with a maximum of eighty (80) persons within any given acre. These numbers constitute a 60 percent greater allowance than the intensity limits from the 2004 Countywide Policies. (An intensity bonus of 30 percent over the maximum number of persons within any given acre may be allowed if the building design includes features intended to reduce risks to occupants in the event of an aircraft collision with the building.)

Prohibited uses in Compatibility Zone B1 include children's schools, day care centers, libraries, nursing homes, hospitals, places of worship, buildings with more than two above-ground habitable floors, highly noise-sensitive outdoor non-residential uses, aboveground bulk storage of hazardous materials, critical community infrastructure facilities, and hazards to flight. Additionally, structures must be located a maximum distance from the extended runway centerline, a minimum noise level reduction of 25 decibels ("dB") must be achieved in residences (including mobile homes) and office buildings, and airspace review is required for objects greater than 35 feet in height. Overall, 30 percent of the entire acreage within Compatibility Zone B1 must be set aside as open land. Dedication of avigation easements is required for all future development in Compatibility Zone B1.

Compatibility Zone B2: Compatibility Zone B2 is the Adjacent to Runway Zone. This is land parallel to the runway, rather than the areas regularly overflowed by arriving or departing aircraft. New residential development in Compatibility Zone B2 is limited to 0.1 dwelling units per acre – an average density of one dwelling unit per ten (10) acres. Non-residential development may maintain a maximum intensity of one hundred (100) persons per acre (averaged over the site), with a maximum of three hundred (300) persons within any given acre and eligibility for the 30 percent intensity bonus. The allowable single-acre intensity constitutes a 50 percent greater allowance than the intensity limit from the 2004 Countywide Policies. Prohibited uses in Compatibility Zone B2 are the same as those in Compatibility Zone B1, and the requirements for airspace review and noise level reduction are also the same. Structures in Compatibility Zone B2 must be located a maximum distance from the runway. Compatibility Zone B2 does not have an open land requirement. Dedication of avigation easements is required for all development in Compatibility Zones A, B1, and B2.

Compatibility Zone C: Compatibility Zone C includes the Extended Approach/Departure Zone and the Turning Zone as described in the *Handbook*. This area extends out from Compatibility Zones A and B1 as one moves farther out from the runway ends. New residential development in Compatibility Zone C is limited to 0.2 dwelling units per acre – an average density of one dwelling unit per five (5) acres. Non-residential development may maintain a maximum intensity of one hundred (100) persons per acre (averaged over a site), with a maximum of two hundred fifty (250) persons within any given acre and eligibility for the 30 percent intensity bonus. The allowable average intensity constitutes a 33.3 percent greater allowance than the intensity limit from the 2004 Countywide Policies, while the allowable single-acre intensity constitutes a 66.7 percent greater allowance. Prohibited uses in Compatibility Zone C include children's schools, day care centers, libraries, nursing homes, hospitals, buildings with more than three aboveground habitable floors, highly noise-sensitive outdoor non-residential uses, and hazards to flight. A minimum noise level reduction of 20 dB must be achieved in residences (including mobile homes) and office buildings, and airspace review is required for objects greater than 70 feet in height. Overall, 20 percent of the entire acreage within Compatibility Zone C must be set aside as open land.

Compatibility Zone D: Compatibility Zone D is the Primary Traffic Patterns and Runway Buffer Area. Pursuant to the 2004 Countywide Policies, new residential development in Compatibility Zone D is limited to either a maximum density of 0.2 dwelling units per acre (average density of one dwelling unit per five (5) acres) or a minimum density of not less than five (5) dwelling units per acre. Intermediate density levels greater than 0.2, but less than 5.0, dwelling units per acre are prohibited. However, most of the vacant residential land within this Compatibility Zone is designated for densities in the prohibited intermediate density range. Therefore, the Plan incorporates Additional Compatibility Policy 2.3, which would allow for residential densities of 4.5 dwelling units per acre or greater, as well as densities up to 0.4 dwelling units per acre. Non-residential development may maintain a maximum intensity of two hundred (200) persons per acre (averaged over a site), with a maximum of eight hundred (800) persons within any given acre. The allowable average intensity constitutes a 100 percent greater allowance than the intensity limit from the 2004 Countywide Policies, while the allowable single-acre intensity constitutes a 166.67 percent greater allowance. (An intensity bonus of 30 percent over the maximum number of persons within any given acre may be allowed if the building design includes features intended to reduce risks to occupants in the event of an aircraft collision with the building.) Children's schools, hospitals, and nursing homes are discouraged in Compatibility Zone D, while highly noise-sensitive outdoor non-residential uses and hazards to flight are prohibited. Airspace review is required for objects greater than 70 feet in height, and 10 percent of the entire acreage within Compatibility Zone D must be set aside as open land.

Compatibility Zone E: Compatibility Zone E comprises the remainder of the Airport Influence Area. There are no general limitations on residential density or nonresidential intensity in this Compatibility Zone and no open area requirements. Hazards to flight are prohibited, and major spectator-oriented sports stadiums, amphitheatres, and concert halls are discouraged.

Based upon an evaluation of City of Hemet land use designations, including Specific Plan designations, and County land use designations within the AIA, inconsistencies have been identified

between the Compatibility Plan criteria and the City's and County's existing General Plan land use designations. To the extent that adoption of the Compatibility Plan would result in conflicts between the Compatibility Plan and the City's and County's General Plans, such conflicts would necessitate either amendments to the local jurisdiction's General Plans or overrules by the applicable governing bodies (the Hemet City Council or the Riverside County Board of Supervisors). (Notably, these local jurisdictions are required by state law to make their General Plans consistent with an adopted ALUCP, or otherwise exercise the overrule provisions set forth in the Public Utilities Code.)

FUNDAMENTALS:

The fundamental purpose of the Commission is to promote land use compatibility around airports. Indeed, section 21670 of the State Aeronautics Act defines the purpose of that statutory scheme, which is "to protect public health, safety, and welfare by ensuring the orderly expansion of airports and the adoption of land use measures that minimize the public's exposure to excessive noise and safety hazards within areas around public airports to the extent that these areas are not already devoted to incompatible uses."

The Commission is vested with certain powers and duties in order to accomplish the purposes and objectives set forth in the State Aeronautics Act, including the preparation and adoption of an ALUCP to address concerns related to potential impacts associated with exposure to aircraft noise, protection of public safety with respect both to people on the ground and the occupants of aircraft, protection of airport airspace, and general concerns with aircraft overflight. The Commission "shall be guided by" information in the *California Airport Land Use Planning Handbook*. (Pub. Util. Code, §21674, subd. (a).) Thus, the Hemet ALUCP is based on the principles in the *Handbook*.

It should be noted that the density and intensity ranges incorporated in the Countywide land use compatibility criteria are not specifically mandated by state law; therefore, the Commission has the discretion to adopt a Plan that incorporates the proposed Additional Compatibility Policies, which would establish airport-specific criteria that are not presently applicable to the majority of airport influence areas in Riverside County.

The task of the Commission is to adopt an ALUCP that provides for the continued operation and orderly growth of aviation-related activities at Hemet-Ryan Airport, while at the same time protecting the public health, safety, and welfare from aircraft noise and hazards from potential aircraft accidents. This task is to be met in a cooperative effort with the local jurisdictions and with an understanding of the needs of the surrounding community. It is in the interests of all parties that an appropriate balance be achieved in this effort.

The 2004 Riverside County Airport Land Use Compatibility Plan Countywide Policies were prepared in accordance with the 2002 *California Airport Land Use Planning Handbook*, but in some cases the criteria were more stringent than the *Handbook* mandated. For example, the *Handbook* does not require limitations of residential density in the Traffic Pattern Zone (*i.e.*, Compatibility Zones D and E). Additionally, the recommended *Handbook* criteria allowed higher non-residential intensities in the Inner and Outer Approach/Departure Zones and the Traffic Pattern Zone than the

criteria utilized in the 2004 Countywide Policies, and the differences between the 2004 Countywide Policies allowances and the State's allowances increased with the publication of the 2011 *Handbook*.

EFFECT ON LAND USE DESIGNATIONS:

The Initial Study/Negative Declaration document includes analyses of the effects of the Compatibility Plan on land use and planning with Additional Compatibility Policies 2.1, 2.2, and 2.3.

Non-Residential Intensity:

The Hemet ALUCP has the potential to constrain non-residential land use development envisioned in the City of Hemet General Plan by applying intensity limitations to properties designated for commercial and industrial development. Most of the potential displacement would occur in Compatibility Zone B1, with a 65 percent reduction in Community Commercial, 54 percent reduction in Industrial, and 48 percent reduction in Business Park square footage. With the proposed Additional Compatibility Policies, there is no potential displacement of retail, office, manufacturing, or warehousing uses in Compatibility Zone D.

Additional Compatibility Policy 2.2, rather than establishing a revised density or intensity number in terms of persons per acre, establishes a revised assumption to be used in calculating the concentration of people in retail establishments. Additional Compatibility Policy 2.2 establishes new square foot per person criteria for retail sales, display, and showroom areas of one person per 115 square feet of gross floor area (without the 50 percent reduction allowed under the maximum occupancy method).

Residential Densities:

By constraining development at some locations, the Hemet ALUCP has the potential to preclude development of new dwelling units. Based upon an evaluation of the City of Hemet General Plan's land use designations, including Specific Plan designations, and the County of Riverside's land use designations within the Airport Influence Area, the Hemet ALUCP would conflict with existing land use designations by applying density limitations to future residential development in Compatibility Zones B1, C, and D. Under a "worst-case scenario," adoption of the Hemet ALUCP may trigger General Plan amendments and Specific Plan amendments that would preclude build-out of up to 3,961 dwelling units that potentially could be developed pursuant to the City of Hemet and County of Riverside General Plans.

The potential displacement of future residential development includes 3,796 dwelling units within the City of Hemet's existing boundaries and 165 dwelling units in the unincorporated area. Under this "worst-case" scenario, approximately 72 percent of the maximum potential residential displacement occurs in Compatibility Zone D. However, if the City is willing to guide development in the areas designated for densities of 2 to 5 dwelling units per acre in Compatibility Zone D to propose projects with net densities of 4.5 to 5.0 dwelling units per acre, the potential displacement of dwelling units in the City could be reduced to as little as 1,100 dwelling units. In that scenario, 80

percent of the potential residential yield displacement would occur in Compatibility Zone C.

Considering only the area within the City of Hemet, 992 of the potentially displaced units would be within Compatibility Zone C and 108 would be within Compatibility Zone B2. However, the 22.01-acre area with a City General Plan land use designation of Low Density Residential (2 to 5 dwelling units per acre) located in Compatibility Zone B2 is a portion of the Hemet West Mobile Home Park. If that community continues into the future with no additional spaces proposed in the Zone B2 area, the potential displacement in the City is reduced by 108 dwelling units. Additionally, 10.05 acres of the 26.39 acres with a City General Plan land use designation of High Density Residential located in Compatibility Zone C are included in the existing 358-space Casa Del Sol Recreational Vehicle Park. If that community continues into the future with no additional spaces proposed in the Zone C area, the potential displacement in the City is reduced by 299 dwelling units. These two areas account for 37 percent of the potential residential displacement from the B2 and C Zones in the City of Hemet.

COMMENTS ON THE ENVIRONMENTAL DOCUMENT:

For the Hemet-Ryan project, staff has received several phone calls from citizens who pointed out that the notice is not understandable. The reference to a "Negative Declaration" is one of the more confusing aspects for the public. This simply means that the proposed project does not require an Environmental Impact Report.

In accordance with CEQA, staff distributed a "Notice of Intent to Adopt" the Initial Study/Negative Declaration (IS/ND), and received public comments on the IS/ND for a 30-day period, beginning November 7, 2016 and concluding with this hearing. As of November 15, ALUC staff had been contacted by several people who received copies of the Notice of Intent. Generally, the recipients were concerned about the potential effect of the adoption of the Plan on the values of their properties.

RESOLUTION NO. 2016-03

**A RESOLUTION OF THE RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
ADOPTING THE NEGATIVE DECLARATION FOR THE 2016 HEMET-RYAN AIRPORT LAND
USE COMPATIBILITY PLAN AND ADOPTING THE 2016 HEMET-RYAN AIRPORT LAND USE
COMPATIBILITY PLAN**

WHEREAS, the California Public Utilities Code requires each county in the state with an airport or landing strip operated for the benefit of the general public to establish an Airport Land Use Commission ("ALUC"), the goals of which are to promote public health, welfare, and safety by ensuring compatible land uses around airports (see Pub. Util. Code, §21670 et seq.); and,

WHEREAS, in December 1970, after a duly noticed public hearing, the Riverside County Board of Supervisors, acting in conjunction with the mayors of the cities in the county, designated the then-existing five member Riverside County Aviation Commission to assume the planning responsibilities of an ALUC; in 1982, the Board of Supervisors augmented the ALUC with two members selected by the committee of mayors; and, in September 1997, the Board of Supervisors reformed the ALUC pursuant to the Public Utilities Code, as amended; and,

WHEREAS, Public Utilities Code section 21675 provides that the ALUC shall prepare and adopt an Airport Land Use Compatibility Plan ("ALUCP"), formerly known as a Comprehensive Land Use Plan, for each operating, public use airport within its jurisdiction, and further specifies that each ALUCP shall contain land use planning guidelines to promote compatible land use development in the areas surrounding each airport, to the extent the land is not already devoted to incompatible land uses; and,

WHEREAS, the ALUC has prepared the proposed Hemet-Ryan Airport ALUCP ("2016 Hemet-Ryan Plan"; Riverside County Airport Land Use Commission Case No. ZAPEA02HR13), which establishes new boundaries of the airport influence area ("AIA") for Hemet-Ryan Airport and addresses land use compatibility in the AIA for Hemet-Ryan Airport; and,

WHEREAS, pursuant to the Public Utilities Code section 21647.7, subdivision (a), the formulation, adoption, and amendment of an ALUCP shall be guided by information contained in the Airport Land Use Planning Handbook ("Handbook") published by the Division of Aeronautics of the California Department of Transportation ("Caltrans"); and,

WHEREAS, at the direction of the ALUC, staff prepared an Initial Study for the proposed 2016 Hemet-Ryan Plan, pursuant to the California Environmental Quality Act ("CEQA;" Pub. Resources Code, §21000 et seq.), to determine if the 2016 Hemet-Ryan Plan may have a significant effect on the environment. The 2016 Hemet-Ryan Plan includes, but is not limited to, land use policies and guidelines to be used to determine whether a proposed land use is compatible with the operation of Hemet-Ryan Airport, airport layout diagrams, figures identifying the airport's noise contours and safety zones; and,

WHEREAS, the Initial Study showed that there is no substantial evidence, in light of the whole record, that the proposed 2016 Hemet-Ryan Plan may have a significant effect on the environment; and,

WHEREAS, based on the Initial Study, a proposed Negative Declaration was prepared, pursuant to CEQA, which documented the reasons in support of the finding that the 2016 Hemet-Ryan Plan would not have a significant effect on the environment; and,

WHEREAS, the ALUC prepared a Notice of Intent ("NOI") to Adopt a Negative Declaration for the 2016 Hemet-Ryan Plan, which: (i) provided notice of the ALUC's intention to adopt a Negative Declaration for the 2016 Hemet-Ryan Plan; (ii) stated that the ALUC would receive public comments on the proposed Negative Declaration for a 30-day period, beginning November 7, 2016 and concluding on

December 8, 2016; (iii) identified the locations where copies of the Initial Study and proposed Negative Declaration were available for review, which included the ALUC's website; and, (iv) noted that the ALUC would hold a public hearing to consider adoption of the 2016 Hemet-Ryan Plan and the proposed Negative Declaration on December 8, 2016; and,

WHEREAS, on or before November 3, 2016, the ALUC transmitted copies of the NOI to all owners of properties within proposed Compatibility Zones A, B1, B2, C, and D whose development rights could be affected by the proposed Plan (excluding owners of existing homes on lots not available for land division pursuant to existing City or County General Plans); and,

WHEREAS, on November 4, 2016, the ALUC (via consultant firm Mead & Hunt, Inc.) delivered copies of the NOI, the Initial Study and the proposed Negative Declaration to the State Clearinghouse for review by state agencies; and,

WHEREAS, on November 7, 2016, the ALUC transmitted an electronic copy of the NOI to the Clerk of the County of Riverside for public posting; and,

WHEREAS, all written comment letters submitted on the Initial Study and Negative Declaration were provided to the ALUC and were made available to the public at the December 8, 2016 public hearing; and,

WHEREAS, a duly noticed public hearing was held before the ALUC on December 8, 2016, at which time all public and affected government agency comments, testimony, and evidence were presented as to the proposed 2016 Hemet-Ryan Plan, Initial Study, and Negative Declaration; and,

WHEREAS, the ALUC has carefully and thoroughly reviewed the Initial Study and proposed Negative Declaration and all public comments and responses pertaining thereto, both written and oral, all of which are incorporated herein by this reference. These documents and other materials, including those documents reviewed and incorporated by reference, constitute the record of proceedings on which the ALUC's determination is based, and are in the custody of the staff of the Riverside County ALUC, whose offices are located at 4080 Lemon Street, 14th Floor, Riverside, California, 92501; and,

WHEREAS, the public comments do not constitute substantial evidence in support of a fair argument that the 2016 Hemet-Ryan Plan may have a significant effect on the environment or that preparation of an environmental impact report ("EIR") is required under CEQA; and,

WHEREAS, the purpose of an ALUCP is to protect the public health, safety, and welfare by ensuring the orderly expansion of airports and the adoption of land use measures that minimize the public's exposure to excessive noise and safety hazards within areas of the City of Hemet and unincorporated Riverside County around Hemet-Ryan Airport, to the extent that these areas are not already devoted to incompatible uses; and,

WHEREAS, ALUC staff consulted with and sought comments from affected land use jurisdictions regarding the proposed Negative Declaration, Initial Study, the 2016 Hemet-Ryan Plan and the proposed AIA boundary, as required by Public Utilities Code section 21675, subdivision (c); and,

WHEREAS, based on the entire record of these proceedings, the ALUC has determined that the proposed Negative Declaration should be adopted because there is no substantial evidence that the 2016 Hemet-Ryan Plan may have a significant effect on the environment; and,

WHEREAS, the ALUC has considered all of the information presented to it as set forth above, and this Resolution memorializes the final action taken by the ALUC on December 8, 2016, as a result of the ALUC's independent judgment and analysis.

BE IT RESOLVED, FOUND, DETERMINED, AND ORDERED on December 8, 2016, that the Riverside County ALUC:

1. Certifies that the Initial Study and Negative Declaration (SCH No. 2010111003) have been prepared and completed in compliance with CEQA (Pub. Resources Code, §21000 et seq.) and the CEQA Guidelines (Cal. Code of Regs., tit. 14, §15000 et seq.); and,
2. Finds, on the basis of the whole record before it, that there is no substantial evidence that the 2016 Hemet-Ryan Plan may have a significant effect on the environment, and that the Negative Declaration reflects the ALUC's independent judgment and analysis; and,
3. Adopts the Negative Declaration dated November 2, 2016; and,
4. Adopts the 2016 Hemet-Ryan Plan; and,
5. Directs staff to prepare a final printing of the 2016 Hemet-Ryan Plan, as adopted, and to send a copy to the State of California Department of Transportation, Division of Aeronautics; the Offices of the City Manager and the Director of Community Development of the City of Hemet; and the Riverside County Planning Department; and,
6. Directs staff to file a Notice of Determination pursuant to CEQA and the CEQA Guidelines.

NOW, THEREFORE, the Riverside County ALUC hereby adopts this Resolution by the following vote:

AYES: Commissioners:

NAYS: Commissioners:

Chair,
Riverside County ALUC

WITNESS, my hand this eighth day of December, 2016.

Director,
Riverside County ALUC

Discussion of Unresolved Issues

Draft Hemet-Ryan Airport Land Use Compatibility Plan

No.	Issue	Discussion	Action Choices
1	<p>Zone D Residential Density: The 11/16 draft ALUCP allows residential development at high and low densities, but prohibits densities in the mid-range between 2½-acre lots and 4.5 dwelling units per acre. Hemet would like to have densities of 3 d.u./ac. be allowed at the bottom of the higher-density range or, preferably, to have all density restrictions eliminated. The city would be willing to have aviation easement dedication be required in Zone D.</p>	<p>Early drafts of the 2004 countywide policies limited residential development to lot sizes of 5 acres or more. The intent was to minimize residential uses in areas regularly overflown by aircraft in the traffic patterns. Noise and annoyance were the concerns, not safety. Prior to adoption, the high-density option was introduced. The concept behind this option is that high-density, multi-family residential uses have higher ambient noise levels and less outdoor living and thus residents are less likely to be bothered by aircraft overflights. Originally, only densities above 8 d.u./ac. were to be allowed, but as adopted, "high" densities as low as 5 d.u./ac. are allowed under the countywide policies.</p> <p>Further lowering of the bottom of the high-density option largely defeats its purpose in that it would allow mid-range, single-family densities at which overflight noise is most often annoying to residents. The city, though, contends that extensive residential development at this density already exists in Zone D and that few noise complaints occur.</p> <p>Changing the low-density option to allow lots as small as 2½ acres, also chips away at the purpose of avoiding densities where annoyance is most common. At some point, precluding densities only in a narrow range becomes unsupportable. Further evaluation of the high-low density policy is planned to take place as part of the countywide policy update project in 2017. At present, though, reducing the unallowable density range or eliminating it altogether would make the Hemet ALUCP policy different from what is applied at the other airports in the county.</p> <p>The countywide policies already require deed notices for new development in Zone D. Deed notices serve to alert prospective buyers that the property is subject to aircraft overflight. Aviation easements also accomplish this purpose, but add height limit restrictions that are not needed in most of Zone D.</p>	<p>M&H recommends that the ALUC adopt the high-low density policy as currently proposed, thus acknowledging that there are overflight issues. If the city wants to have single-family homes in this area, then they can overrule the ALUC. If the draft Hemet policy is adopted as is and the 2017 countywide update process results in a policy allowing greater density options in Zone D, the Hemet ALUCP policy could be amended at that time.</p> <p>M&H does not favor further narrowing the unacceptable density range. Eliminating all density restrictions for Zone D would be easier to support.</p>
2	<p>Open Land: Hemet would like to reduce or eliminate the 10% open land requirement for Zone D.</p>	<p>The purpose of the open land requirement is to ensure that places remain available in the airport vicinity where the pilot of an aircraft in distress can make a survivable emergency landing. The need for open land is greatest close to the runway where the low altitude of the aircraft limits the options for an emergency landing, but is still important elsewhere within the traffic pattern. At present, a considerable amount of land remains open around the airport. However, without adequate advance planning, open land sites that are large enough to be useful for an emergency landing will disappear as development takes place in the airport vicinity.</p> <p>Although not clearly spelled out in the countywide policies, the preferred way of ensuring adequate open land in the vicinity of an urbanizing airport environs is to identify suitable locations in a comprehensive manner rather than as individual development projects are proposed. This identification of open land could take place as part of the ALUCP adoption action or later as part of the general plan adoption process. At this point, Hemet has</p>	<p>Given the nearness of the desired adoption date for the Hemet ALUCP, M&H suggests leaving the current open land requirement in place. More time will be available to identify and evaluate open land sites as part of the later consistency review process for the Hemet general plan.</p> <p>To clarify this intent and put more onus on the city to</p>

		mentioned several locations that could provide open land, but no comprehensive mapping of the airport environs has been completed. Most of the sites mentioned are public lands or rights-of-way. These sites alone probably would not come close to meeting the 10% requirement. Future large subdivisions thus would also need to contribute through wide roadways, linear parks, or in other manners.	comply, consider adding the following HMT-specific policy: The ALUC shall not make a determination of consistency for any major land use action in the Compatibility Zone D of the Hemet-Ryan Airport influence area until the City of Hemet has mapped suitable open lands as required by Policy 4.2.4 and this mapping has been accepted by the ALUC. If the identified sites are sufficiently large and well-positioned relative to aircraft flight routes to provide enhanced safety, the ALUC may accept an amount of open land in Zone D that is less than 10%.
3	<i>Eastern Zones B1 and C Dimensions:</i> Hemet and a landowner would like to have the eastern ends of Zones B1 and C along the extended runway centerline each be moved westward by 1,000 feet to match the safety zone lengths recommended in the Caltrans Handbook.	As currently drafted, Compatibility Zone B1 would extend 5,000 feet from the existing eastern end of the runway which in the future is proposed to become the location of a displaced landing threshold. Zone C extends an additional 3,000 feet. The Handbook safety zone guidance for a medium-length runway such as Hemet's has Zones B1 and C lengths of 4,000 feet and 3,000 feet respectively. Unlike the Handbook, however, the Hemet ALUCP zones also take noise into account. The B1 and C zones each are extended an additional 1,000 feet from the runway so as to almost fully enclose the CNEL 60 dB and 55 dB noise contours, respectively. Examination of the Hemet general plan land use map (Exhibit HR-9) indicates that the area that would be removed from Zone B1 if it is shortened is planned for commercial and business park uses. The area is currently undeveloped. The affected area in Zone C is designated as commercial and high-density residential, but appears to be fully developed. Given that the rationale for the additional 1,000-foot lengths of these zones is noise based, making the requested changes would not create a potential for significant future compatibility conflicts. Alternatively, the outer end of Zone B1 could remain in its current location to provide additional safety protection—40 people/acre in Zone B1 as opposed to 100 people/acre in Zone C.	There are three choices for the ALUC to pick from: 1) Leave zones as in draft ALUCP. 2) Leave outer end of Zone B1 as in draft and move Zone C end inward by 1,000 feet. 3) Bring ends of both zones inward by 1,000 feet. M&H suggests Option 2. However, also see Issue #7 raised by Caltrans.
4	<i>Infill Development:</i> Hemet would like to have some areas of future development that might conflict with ALUCP criteria treated as infill.	Countywide Policies 2.2.1(c) and 3.3.1 provide for this option. The burden of identifying possible infill areas rests with the local agency, in this case the City of Hemet. Infill areas should be mapped as part of the general plan consistency process and are subject to concurrence by the ALUC.	No action needed at this time.

5	<p><i>Calculating Usage Intensity:</i> Hemet has asked for clarification on how usage intensities (people/acre) are to be calculated for nonresidential land uses.</p>	<p>Guidance on how the ALUC calculates usage intensities is provided in Appendix C of the Countywide Policies document. An indication of the likely compatibility status of individual types of land uses is presented in Appendix D. The Appendix C methodology primarily relies on parking ordinances and building codes as a means of estimating the maximum number of people that a land use typically attracts.</p> <p>Other methodologies can also be used and some will be further explored as part of the countywide policies update in 2017. One such methodology is introduced in draft Hemet Policy HR-2.2. It utilizes pre-defined occupancy levels—the number of square feet per person—as a way of estimating the number of people in a building or on a site. For buildings, the occupancy level for various types of land uses, together with the usage intensity criteria, can be used to mathematically calculate a maximum floor area ratio (FAR) for that use in a particular compatibility zone.</p>	<p>No policy changes necessary at this time. ALUC staff to work with city to assist them in using current methodology. Evaluation of expanding use of occupancy levels and FARs to be done in countywide policy update.</p>
6	<p><i>Site-Specific Exceptions:</i> Hemet has mentioned that they may seek site-specific exceptions for certain prospective developments that may not meet the new compatibility criteria.</p>	<p>The city has not yet provided a list of these developments. Establishment of site-specific exceptions may be reasonable for development proposals that the city has under active consideration, but which have not yet received approval, if the development would be consistent under the currently adopted ALUCP but not under the draft ALUCP. For any site-specific exceptions to be considered, enough detail will need to be available to assess the extent to which non-compliance would occur and what measures are to be applied to minimize the significance of the conflicts.</p>	<p>No action proposed at this time. Waiting for further input from the city.</p>
7	<p><i>Consistency with Caltrans Handbook Guidance:</i> In a 11/16/2016 telephone conference call, Caltrans Aeronautics expressed concern that the draft Hemet compatibility zones do not fully encompass the equivalent Handbook safety zones.</p>	<p>Using Exhibits HR-7 and HR-8, M&H explained the rationale for the composite—noise as well as safety—zones used in the draft ALUCP and Caltrans was generally accepting of the explanation with regard to the width of Compatibility Zone B1. One remaining area about which they seemed to still have concern is that draft ALUCP Compatibility Zone C on the south side of the eastern runway end does not fully encompass Handbook Safety Zone 3. The southernmost corner of this area is already occupied by residential development and the portion north of the drainage channel is mostly vacant land planned for commercial and business park uses.</p>	<p>Written comments from Caltrans are pending. If this disparity continues to be an issue with them, ALUC can consider extending Zone C by 500 feet to encompass the area not in Handbook Safety Zone 3.</p>

THE PROPOSED PLAN

DRAFT

HEMET-RYAN AIRPORT LAND USE COMPATIBILITY PLAN

November 2016

OVERVIEW

This draft *Hemet-Ryan Airport Land Use Compatibility Plan (Hemet ALUCP)* has been prepared for consideration and adoption by the Riverside County Airport Land Use Commission (RCALUC). In accordance with provisions of the California State Aeronautics Act (Public Utilities Code Section 21670 *et seq.*), the RCALUC has been assigned the lead responsibility for airport land use compatibility planning around each of the public-use and military airports in Riverside County, including the preparation of an ALUCP for each airport.

Beginning in 2004, the RCALUC began adopting new versions of the ALUCPs for most of these airports. Each of these individual ALUCPs is contained within a single, countywide document entitled *Riverside County Airport Land Use Compatibility Plan*. The ALUCP for each airport consists of the policies in Chapter 2 of that document that are applicable to all of the airports in the county together with airport-specific policies and maps in Chapter 3. This material plus an introductory chapter (Chapter 1) and a set of appendices comprise Volume I. Background data regarding each airport and its environs is included in Volumes 2 and 3.

This *Hemet-Ryan ALUCP* maintains this established format. Thus, only the policies and maps specific to Hemet-Ryan Airport for insertion into Chapter 3 and the background data to be added to Volume 2 are presented here. All of the countywide policies in Chapter 2 of Volume 1 are considered to be part of the *Hemet-Ryan ALUCP* unless explicitly modified or supplemented by the policies specific to Hemet-Ryan Airport. The introductory and appendix content is also applicable although no ALUC policy is included therein.

This page left intentionally blank.

Insert for Riverside County ALUCP, Volume 1, Chapter 3, Individual Airport Policies and Compatibility Maps

HR. HEMET-RYAN AIRPORT

HR.1 Compatibility Map Delineation

1.1 *Airport Master Plan and Airport Layout Plan Status:* The last adopted Master Plan for Hemet-Ryan Airport was completed in 1982. Several iterations of a new plan have been drafted, but not adopted as of late 2016. The latest iteration is dated May 2011. Remaining Master Plan issues do not involve the airfield configuration or other factors that would have off-airport land use compatibility implications. The most recent version of the Airport Layout Plan (ALP) drawing is dated May 2013. In September 2015, the FAA agreed that the runway configuration shown on the May 2013 draft ALP is the one that will be pursued in the full update of the ALP drawing set scheduled for 2017. Subsequently, in October 2015, the Caltrans Division of Aeronautics accepted use of this ALP as the basis for the Hemet-Ryan ALUCP.

1.2 *Airfield Configuration:* The existing primary runway, 5-23, measures 4,315 feet in length and the landing thresholds are at the runway ends. The draft Airport Master Plan and ALP propose extending the runway 500 feet eastward, but leaving the Runway 23 (east) landing threshold in its present location. Additionally, declared distances would be established indicating that the usable runway length for takeoffs and landings to the east is to remain at 4,315 feet. The net effect is that the extension will increase the usable runway length to 4,815 feet only for takeoffs to the west.

The 2,040-foot secondary runway, 4-22, on the north side of the primary runway has historically been used primarily for sailplane-related launches and landings. Currently, this runway is seldom used. The draft Airport Master Plan and ALP call for its future closure.

1.3 *Airport Activity:* The draft Master Plan estimates that 69,500 aircraft operations took place at the airport in 2010, the forecast base year. The 2036 forecast is 87,150 operations. Given that there has been little if any activity growth since 2010, the 87,150 operations projection is assumed to represent a forecast horizon of at least 20 years from the adoption date of the *Compatibility Plan* as required by state law. The forecast mix of aircraft types will shift toward more activity by turbo-prop airplanes and helicopters and less by piston-engine airplanes, although the latter will continue to make up 80% of the activity. Jet activity will remain minimal. No changes in the distribution of operations by direction of flight or time of day are anticipated.

1.4 *Airport Influence Area:* Among the determinants of the overall airport influence area, the coverage of the airspace protection area defined by Federal Aviation Regulations (FAR) Part 77 encompasses the areas of concern with respect to noise, safety, and overflight. With one exception, the outer edge of the FAR Part 77 conical zone therefore is used to set the airport influence area boundary established in this *Compatibility Plan* and shown on **Map HR-1**. That exception is to exclude from the airport influence area, the small section of the conical zone that extends into the City of San Jacinto north of the airport. Note as well that the FAR Part 77 zones depicted in **Map HR-2** are dimensioned in relationship to the proposed future

4,815-foot runway length despite the anticipated use of declared distances limiting the effective runway length for landings from and takeoffs toward the east. Specifically, the FAR Part 77 surfaces and the corresponding airport influence area boundary extend 14,000 feet from points 200 feet beyond the existing west end and future east end of the primary runway. All boundaries of Compatibility Zones A through D, however, are measured from the physical ends of the *existing* runway. The airport influence area also encompasses the forecast noise contours shown in **Map HR-3**.

- 1.5 *Airspace Protection Map*: The Airspace Protection Map for Hemet-Ryan Airport as depicted in **Map HR-2** takes into account both the existing and proposed future runway configurations as follows:
- (a) In accordance with federal regulations, the official FAR Part 77 surfaces for all runways are based on the full length of the runway. Displaced thresholds and declared distances are not taken into account. However, when either of the latter components are part of a runway design, other surfaces such as the threshold siting surface normally are above the Part 77 approach surface. The result is that objects potentially can penetrate the approach surface yet not conflict with FAA standards.
 - (b) The proposed eastward extension of the Hemet-Ryan Airport runway would have a displaced threshold located where the runway currently ends. Also, declared distances are proposed to be used to limit the operational length of the runway for takeoffs and landings toward the east (Runway 5 direction) to the same distances as the current runway length provides. Because aircraft overflying areas east of the runway would do so at an altitude no lower with the proposed extension than with the existing runway, the height limits in this area thus are fundamentally the same in both instances.
 - (c) The approach and transitional airspace protection surfaces shown in black on **Map HR-2** for the eastern (Runway 23) approach are based on the existing runway end which will become the future displaced threshold location. Shown with a dashed red line are the official surfaces based on the future extended runway end.
 - (d) The preceding FAR Part 77 standards exceptions notwithstanding, the outer edges of the horizontal and conical surfaces to the east are based on the proposed future eastern end of the runway. This assures that all of the official FAR Part 77 surfaces, except for the small portion overlying the city of San Jacinto, are within the Hemet-Ryan Airport Influence Area.

MA.2 Additional/Specific Compatibility Policies

Policies set forth in Chapter 2, Countywide Policies, shall be modified or supplemented for the *Hemet-Ryan ALUCP* as follows.

2.1 *Basic Compatibility Criteria*:

- (a) The maximum intensity criteria listed in **Table 2A** of the Countywide Policies chapter for nonresidential land uses do not apply to the environs of Hemet-Ryan Airport. The compatibility criteria that shall be applicable to the Hemet-Ryan Airport influence area are as follows:

<i>Compatibility Zone</i>	Maximum Intensity (People/Acre)		
	Sitewide Average	Single Acre	With Bonus
<i>A</i>	0 (no change)	0 (no change)	0 (no change)
<i>B1</i>	40	80	104
<i>B2</i>	100 (no change)	300	390
<i>C</i>	100	250	325
<i>D</i>	200	800	1,040
<i>E</i>	No Limit (no change)	No Limit (no change)	No Limit (no change)

(b) Except as modified by other policies in this section, all other provisions of **Table 2A** apply to Hemet-Ryan Airport.

2.2 *Calculation of Concentration of People in Retail Sales Establishments:* The provisions of Table C1 in Appendix C notwithstanding, retail (mercantile) sales and indoor display areas (excluding restaurants and other uses specifically identified separately from retail/mercantile in Table C1) shall be evaluated assuming an occupancy level of 115 square feet of gross floor area per person.

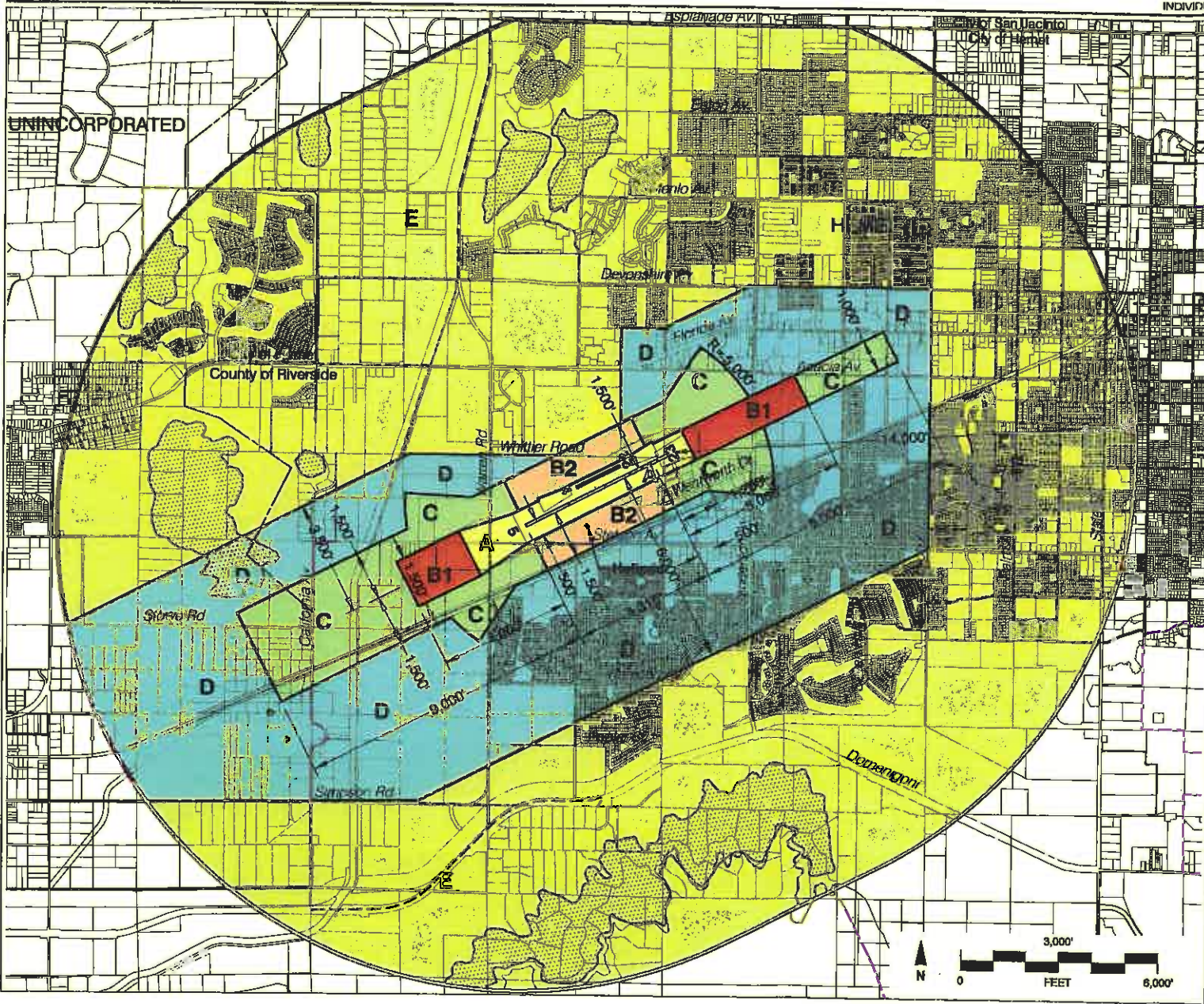
2.3 *Residential Densities in Compatibility Zone D:* The criteria set forth in Countywide Policies 3.13(a) and 3.1.3(b) and the Basic Compatibility Criteria matrix (**Table 2A**) notwithstanding, residential densities greater than or equal to 4.5 dwelling units per net acre and residential densities less than or equal to one dwelling unit per 2½ acres are permitted within Compatibility Zone D.

(a) Residential densities in Compatibility Zone D shall be calculated on a “net” rather than on a “gross” basis.

(b) For the purposes of this *Compatibility Plan*, the net acreage of a project equals the overall developable area of the project site exclusive of permanently dedicated open lands meeting the ALUC open area criteria (as defined in Policy 4.2.4), open space required for environmental conservation purposes, and separate lots used for common areas, public facilities, recreational areas, and drainage basins. Internal streets are counted in the net area, while perimeter streets are not to be included.

2.4 *Airspace Protection Criteria:* Notice to the FAA shall be required for proposed objects near Hemet-Ryan Airport in accordance with the criteria indicated in Federal Aviation Regulations Part 77, Section B, as applied to the proposed future configuration of Runway 5-22 including the planned runway extension. In response to such Notice, the FAA will conduct an Aeronautical Study of the proposed object. If the FAA determines that a proposed object situated within the approach or transitional zones at the east end of the runway object would be an airspace hazard, the ALUC will further investigate the basis for the determination. The expectation of the ALUC is that the surfaces shown in black on **Map HR-2** should be used for the purposes of determining allowable object heights in this area.

This page left intentionally blank.



Legend

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E
- Height Review Overlay Zone

Boundary Lines

- Airport Property Line
- City Limits
- City Sphere of Influence

Note

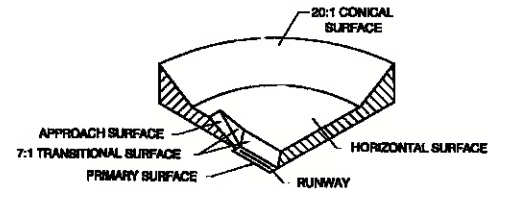
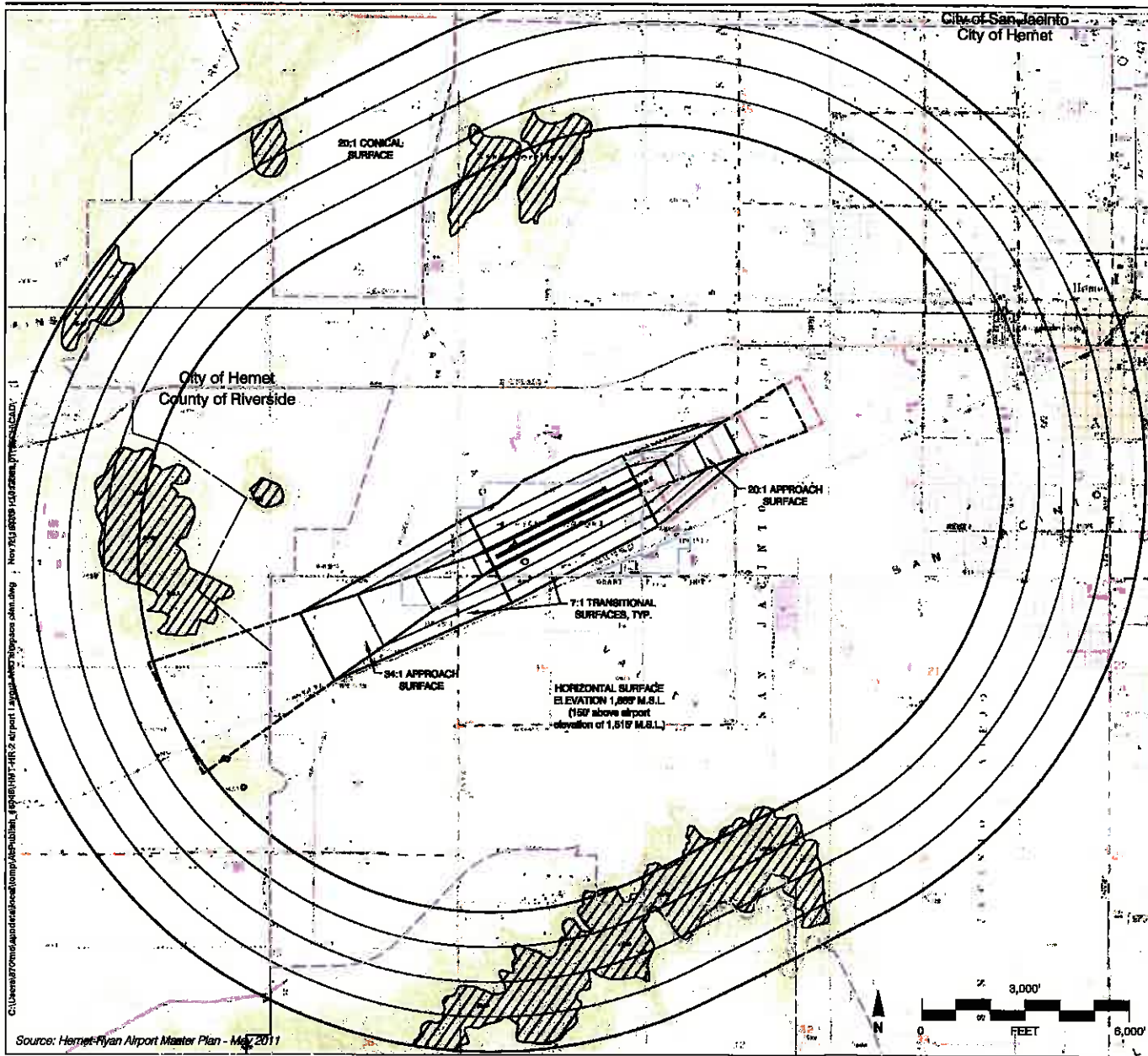
Airport Influence Area boundary measured from a point 200 feet beyond ends of proposed future 4,815 foot runway in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from ends and centerlines of existing 4,315 foot runway.

Riverside County
 Airport Land Use Commission
**Hemet-Ryan Airport
 Land Use Compatibility Plan**
 (November 2016 Draft)

Map HR-1

Compatibility Map
 Hemet-Ryan Airport

C:\Users\lrc\Documents\Projects\Map\Pub\Pub\160611\Map\Compatibility_0116.dwg New 07_2016_03.dwg



TYPICAL FAR PART 77 SURFACES

Notes:

The approach surface for Runway 23 (east end of runway) is adjusted to reflect the proposed use of a displaced threshold and declared distances for the future extended runway.

The horizontal and conical surfaces are based on the proposed future end of the runway.

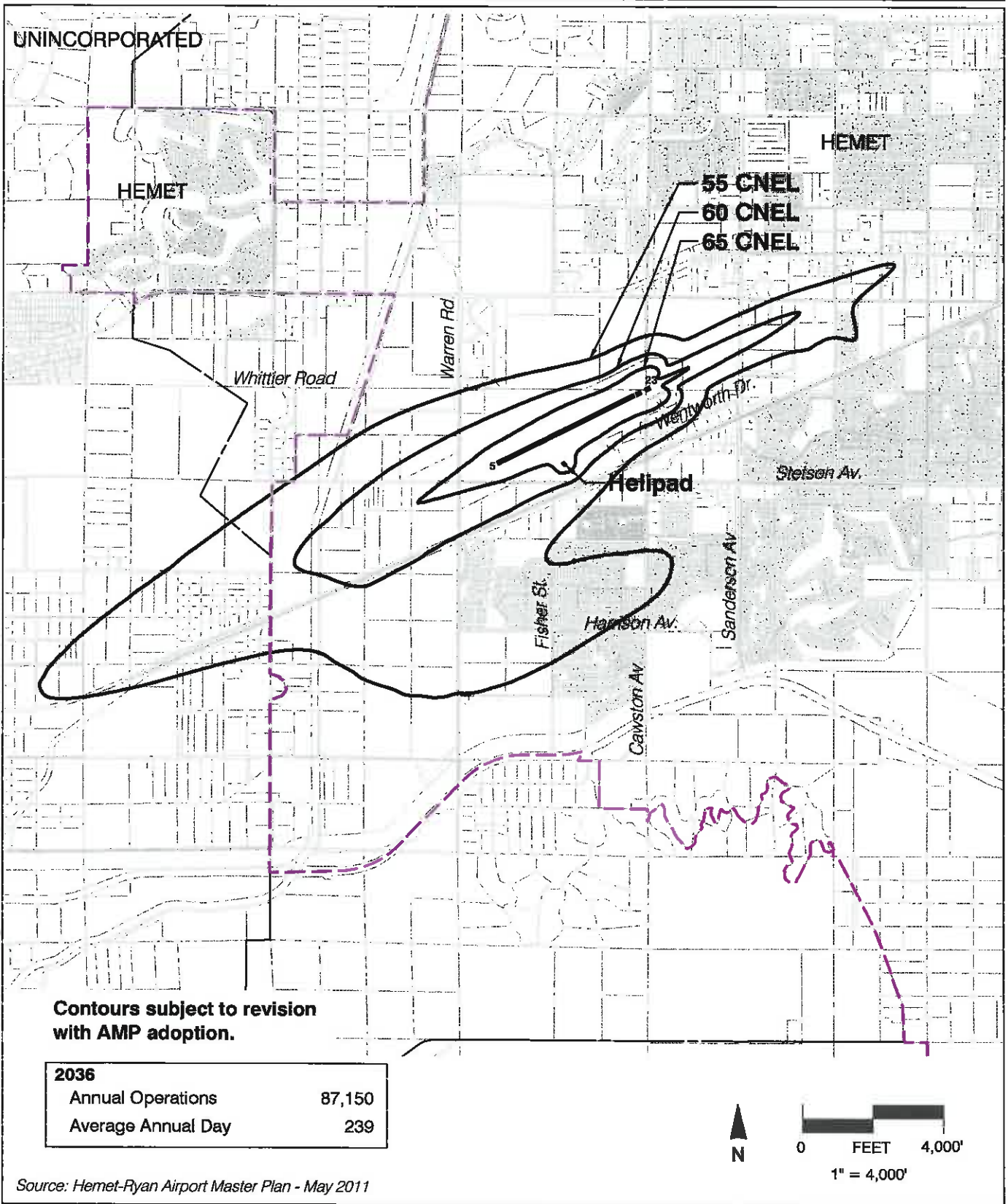
See Policy HR.1 for further explanation and applicable airspace protection policies.

Riverside County
 Airport Land Use Commission
Hemet-Ryan Airport
 Land Use Compatibility Plan
 (November 2016 Draft)

Map HR-2

Airport Airspace Plan
 Hemet-Ryan Airport

Source: Hemet-Ryan Airport Master Plan - May 2011



Map HR-3

**Future Noise Impacts
Hemet-Ryan Airport**

Background Data: Hemet-Ryan Airport and Environs

INTRODUCTION

Hemet-Ryan Airport is owned and operated by the County of Riverside and serves the cities of Hemet, San Jacinto, and other nearby communities in the east-central portion of western Riverside County. The airport sits at an elevation of 1,512 feet in the San Jacinto Valley at the foot of the San Jacinto Mountains. The airport today (2013) comprises 440 acres and has two paved runways. The primary runway, 5-23, is 4,315-feet in length and 100-feet wide. The second runway—designated 4-22 but parallel to the primary runway—is restricted to sailplane and tow plane operations. It is 2,045 feet long and 25 feet wide. Hemet-Ryan Airport provides storage for approximately 150 based aircraft, the majority of which are single-engine piston powered aircraft. A California Department of Forestry and Fire Protection (CalFire) base is located at the airport as well. Total annual aircraft operations, including sailplane operations, were estimated at approximately 70,000 in 2010. More recent data is not available.

Work on a draft Airport Master Plan (AMP) Report for Hemet-Ryan to replace the outdated plan from 1982 commenced in 2010, but has not been completed as of late 2016. The latest draft is dated May 2011. As an interim step, a new Airport Layout Plan (ALP) drawing was prepared in May 2013. While this ALP has not formally been approved by the Federal Aviation Administration, the FAA has preliminarily concurred with the runway configuration as depicted. On this basis, the Caltrans Division of Aeronautics has accepted the May 2013 ALP to serve as the foundation for the present *Hemet-Ryan Airport Land Use Compatibility Plan*.

Airport data in the exhibits that follow in this chapter are based upon material in the 2011 draft AMP and are subject to change when the AMP is adopted. Major proposed airfield changes include extending Runway 5-23 by 500 feet to the east, but keeping the Runway 5 (east) landing threshold in the same location as the current runway end. Also planned is the eventual closure of Runway 4-22. The draft AMP projects the based aircraft population to increase to 175 by 2031. Aircraft operations are projected to reach approximately 87,000 at that time.

Exhibit HR-1 describes current and planned features of the airport. The Airport Layout Plan drawing depicting long-range development is included as **Exhibit HR-2**. **Exhibit HR-3** summarizes data regarding present and future airport activity. Current and projected noise impacts are shown in the two following maps, **Exhibits HR-4** and **HR-5**. **Exhibit HR-6** illustrates the noise contours and overflight area data that are a major component of the Hemet-Ryan Airport Compatibility Zone boundaries also shown on the map. **Exhibit HR-7** shows the risk and airspace protection factors that also contribute to the zone boundaries.

The central area of the city of Hemet lies directly to the east of the airport along the runway approach corridor. The city is expanding westward, both north and south of the airport. Lands to the west remain

generally rural. A summary of information about land uses and land use policies in the airport vicinity is presented in **Exhibit HR-8**. Exhibits **HR-9** and **HR-10** present the planned land uses as found in the general plans of Riverside County and the city of Hemet as of 2012 with the proposed Compatibility Zones of this *Compatibility Plan* overlaid.

GENERAL INFORMATION

- ▶ *Airport Ownership:* County of Riverside
- ▶ *Property Size*
 - › Fee title: 440 acres
 - › Avigation easement: 45 acres
- ▶ *Airport Classification:* General Aviation
- ▶ *Airport Elevation:* 1,512 ft. MSL (surveyed)

BUILDING AREA

- ▶ *Location*
 - › South side of runway
 - › Sailplane facilities north of runways
- ▶ *Aircraft Parking Capacity*
 - › 100 T-hangars/portables
 - › 65 tiedowns
 - › 3 large box hangars
- ▶ *Services*
 - › Fuel: 100LL and Jet-A
 - › Major airframe and powerplant services

RUNWAY/TAXIWAY DESIGN**Runway 5-23**

- ▶ *Airport Reference Code:* B-II
- ▶ *Critical Aircraft:* Citation III
- ▶ *Dimensions:* 4,315 ft. long, 100 ft. wide
- ▶ *Pavement Strength* (main landing gear configuration)
 - › 80,000 lbs. (single wheel)
 - › 130,000 lbs. (double wheel)
- ▶ *Effective Gradient:* 0.25% (rising to east)
- ▶ *Runway Lighting:* Medium-intensity runway edge lighting
- ▶ *Runway Markings:* Nonprecision
- ▶ *Primary Taxiways:* Full-length parallel taxiway on south

Runway 4-22 (to be closed)

- ▶ *Airport Reference Code:* A-I (small)
- ▶ *Critical Aircraft:* Sailplane and towplanes
- ▶ *Dimensions:* 2,045 ft. long, 25 ft. wide
- ▶ *Pavement Strength* (main landing gear configuration)
 - › 5,000 lbs. (single wheel)
- ▶ *Effective Gradient:* 0.29% (rising to east)
- ▶ *Runway Lighting:* None
- ▶ *Runway Markings:* Visual
- ▶ *Primary Taxiways:* Mid-runway connector taxiway only

PROPOSED FACILITY IMPROVEMENTS

- ▶ *Airfield*
 - › 500 ft. runway extension to the east with landing threshold remaining in current location, creating 500 ft. displaced threshold on 4,815-foot runway
 - › 27 acres of land acquisition at east end of runway
- ▶ *Building Area*
 - › CalFire base north side of airfield

APPROACH PROTECTION

- ▶ *Runway Protection Zones (RPZs)*
 - › Runway 5: 500 ft. inner width, 700 ft. outer width, 1,000 ft. long; all on airport property
 - › Runway 23: 500 ft. inner width, 700 ft. outer width, 1,000 ft. long; majority on airport property, balance protected with avigation easement
 - › Runway 4: 250 ft. inner width, 450 ft. outer width, 1,000 ft. long; all on airport property
 - › Runway 22: 250 ft. inner width, 450 ft. outer width, 1,000 ft. long; all on airport property
- ▶ *Approach Obstacles*
 - › Runway 5: none; 50:1 slope clear
 - › Runway 23: none; 50:1 slope clear
 - › Runway 4: none; 50:1 slope clear
 - › Runway 22: Trees 75 ft. tall 1,770 ft. from runway end on centerline; 21:1 slope clear

TRAFFIC PATTERNS AND APPROACH PROCEDURES

- ▶ *Airplane Traffic Patterns*
 - › Runways 5 and 22: Right traffic
 - › Runways 4 and 23: Left traffic
 - › Pattern Altitude: 1,000 ft. AGL
- ▶ *FAR Part 77 Category*
 - › Runway 5: Nonprecision [C]
 - › Runway 23: Visual [B(V)]
 - › Runway 4: Visual [A(V)]
 - › Runway 22: Visual [A(V)]
- ▶ *Instrument Approaches*
 - › Runway 5 GPS: Straight-in nonprecision approach (1 mile visibility; 848 ft. AGL minimum descent height); circling (1 mile visibility, 848 ft. AGL minimum descent height)
- ▶ *Visual Navigational Aids*
 - › Airport: Rotating beacon
 - › Runway 23: 2-light PAPI on left (3.00 degree slope)

AIRPORT PLANNING DOCUMENTS

- ▶ *Airport Master Plan Report*
 - › Last adopted plan dated 1982
 - › Interim draft update, May 2011
 - › New draft anticipated in 2017
- ▶ *Airport Layout Plan Drawing*
 - › Interim draft, May 2013
 - › Preliminary FAA approval, September 2015
 - › Caltrans Division of Aeronautics acceptance as basis for ALUCP, October 2015

Source: Data Compiled by Mead & Hunt, October 2016

Exhibit HR-1

Airport Features Summary

Hemet-Ryan Airport

This page intentionally blank

BASED AIRCRAFT ^a			FLIGHT TRACK USAGE ^a		
<i>Aircraft Type</i>	Current	Future ^b		Current	Future
Single-Engine, Piston	114	125	<i>Single and Multi-Engine Piston</i>		
Multi-Engine, Piston	23	23	Takeoffs		
Turboprop ^c	4	8	Runway 5, Right Downwind Dep.	100%	no
Jets	1	4	Runway 23, Straight Out	100%	change
Helicopter ⁸	10		Landings		
Sailplane	4	0	Runway 5, Straight In	100%	no
<i>Total</i>	<i>154</i>	<i>170</i>	Runway 23, Left Turn Approach	100%	change
<hr/>			<i>Turboprop and Jets</i>		
AIRCRAFT OPERATIONS ^a			Takeoffs		
	Current	Future ^b	Runway 5, Straight Out	100%	no
<i>Total</i>			Runway 23, Straight Out	100%	change
Annual	69,500	87,150	Landings		
Average Day	190	238	Runway 5, Straight In	100%	no
<i>Distribution by Aircraft Type</i>			Runway 23, Straight In	100%	change
Single-Engine, Piston ^d	72%	66%	<i>Sailplanes</i>		
Multi-Engine, Piston	18%	14%	Takeoffs		
Turboprop	7%	14%	Runway 4, Left Downwind Dep.	100%	no
Jets	<1%	<1%	Runway 22, Right Turn Dep.	100%	ops
Helicopter ³	6%		Landings		
<i>Distribution by Type of Operation ^a</i>			Runway 4, Left Turn Approach	100%	no
Local (touch-and-goes)	36%	34%	Runway 22, Right Turn Approach	100%	ops
Itinerant	64%	66%	<i>Helicopters</i>		
<hr/>			Takeoffs		
TIME OF DAY DISTRIBUTION ^a			Helipad, South Departure	100%	no
	Current	Future	Landings		
<i>All Aircraft</i>			Helipad, North Approach	100%	change
Day (7 am to 7pm)	93%	no	<i>Single-Engine Touch-and-Goes</i>		
Evening (7 pm to 10 pm)	5%	change	Runway 5, Right Pattern	100%	no
Night (10 pm to 7 am)	2%		Runway 23, Left Pattern	100%	change
<hr/>			Runway Use Distribution ^a		
	Current	Future			
<i>All Aircraft (except sailplanes)</i>					
Takeoffs & Landings					
<i>Day & Night</i>					
Runway 5	5%	no			
Runway 23	95%	change			
<i>Sailplanes</i>					
Takeoffs & Landings					
<i>Day & Night</i>					
Runway 4	5%	no			
Runway 22	95%	activity			
<hr/>					
NOTES					
^a Source: Hemet-Ryan Airport Master Plan – May 2011 Draft					
^b Master Plan data indicates "Current" year is 2010 and "Future" is 2031; for <i>Compatibility Plan</i> purposes, "Future" is considered to represent 20+ years from date of <i>Compatibility Plan</i> adoption					
^c Based turboprop counts include two CalFire S-2Ts and one OV-10 during the May-October fire season					
^d Existing single-engine activity includes sailplane operations which are exclusive to Runway 4-22; no sailplane activity is included in future activity					

Exhibit HR-3

Airport Activity Data Summary

Hemet-Ryan Airport

This page intentionally blank

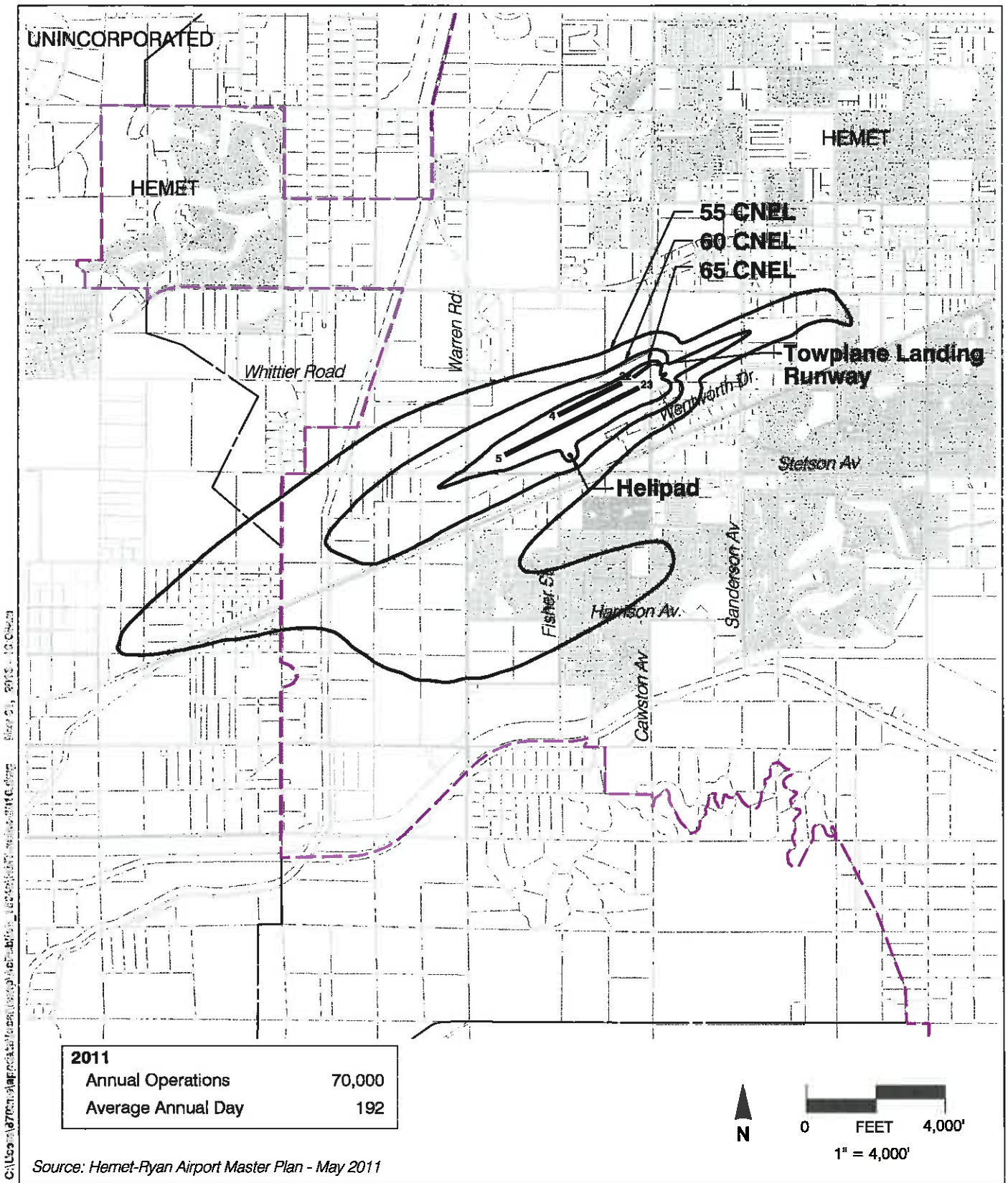
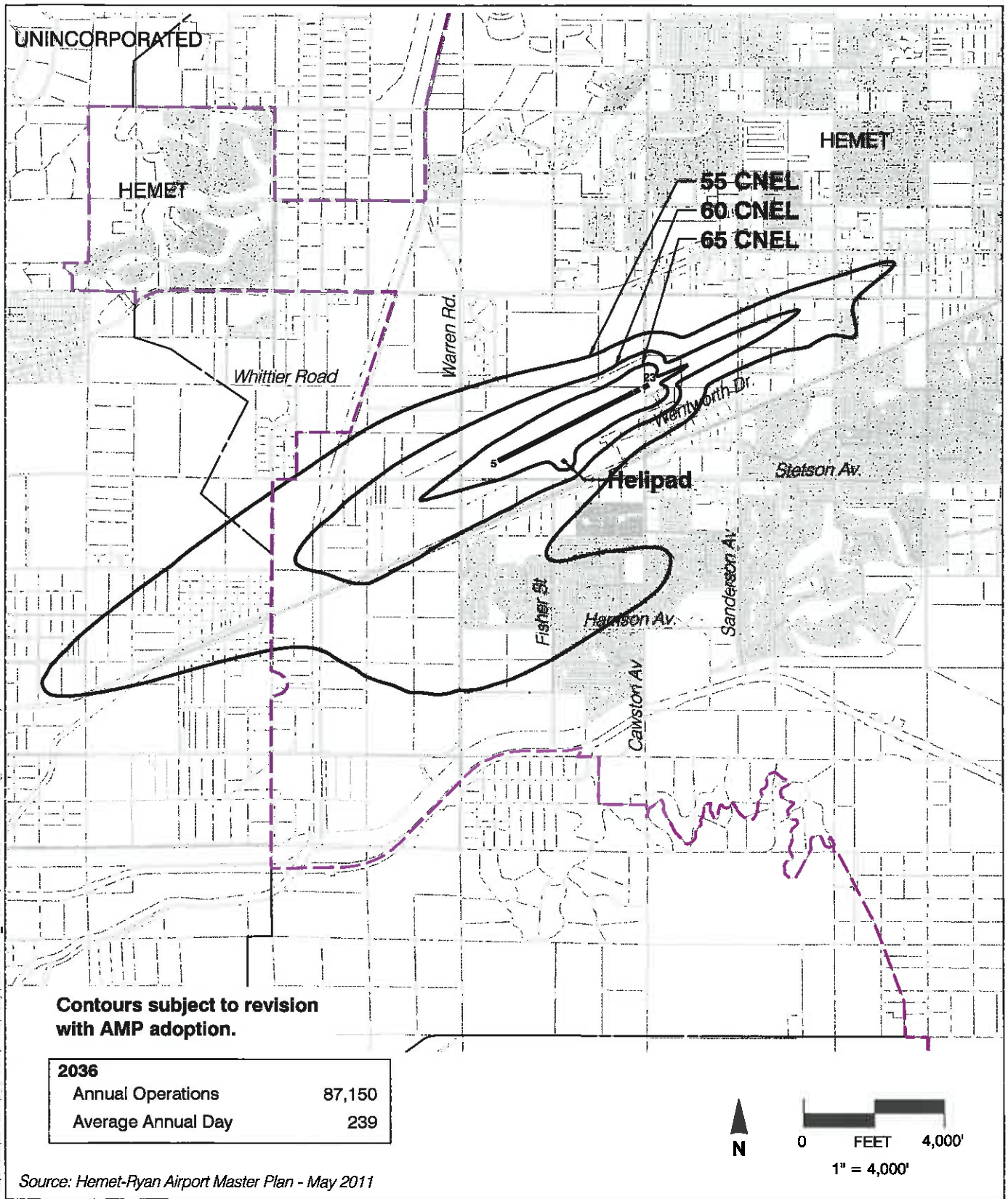


Exhibit HR-4

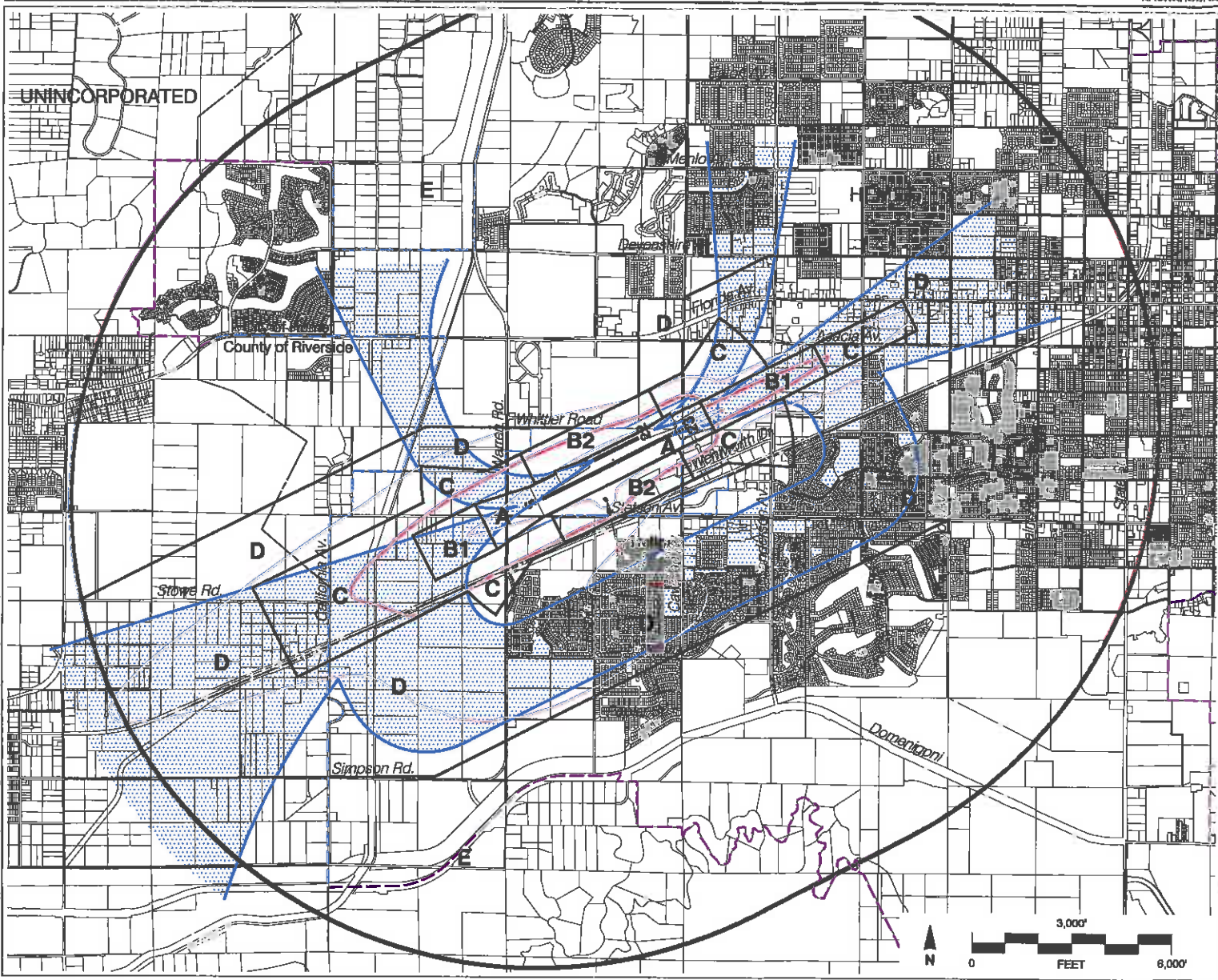
**Existing Noise Impacts
Hemet-Ryan Airport**



C:\Users\5701\OneDrive\data\map_data\Pub\Map_120516\Figures\W5-2\Fig.C.dwg Plot C: 2016 - 10-07am

Exhibit HR-5

**Future Noise Impacts
Hemet-Ryan Airport**



Legend

Noise and Overflight Compatibility Factors

- 55 dB CNEL
- 60 dB CNEL
- 65 dB CNEL

87,160 Future Annual Operations

General Traffic Pattern Envelope (approximately 80% of powered aircraft overflights estimated to occur within these limits)

Boundary Lines

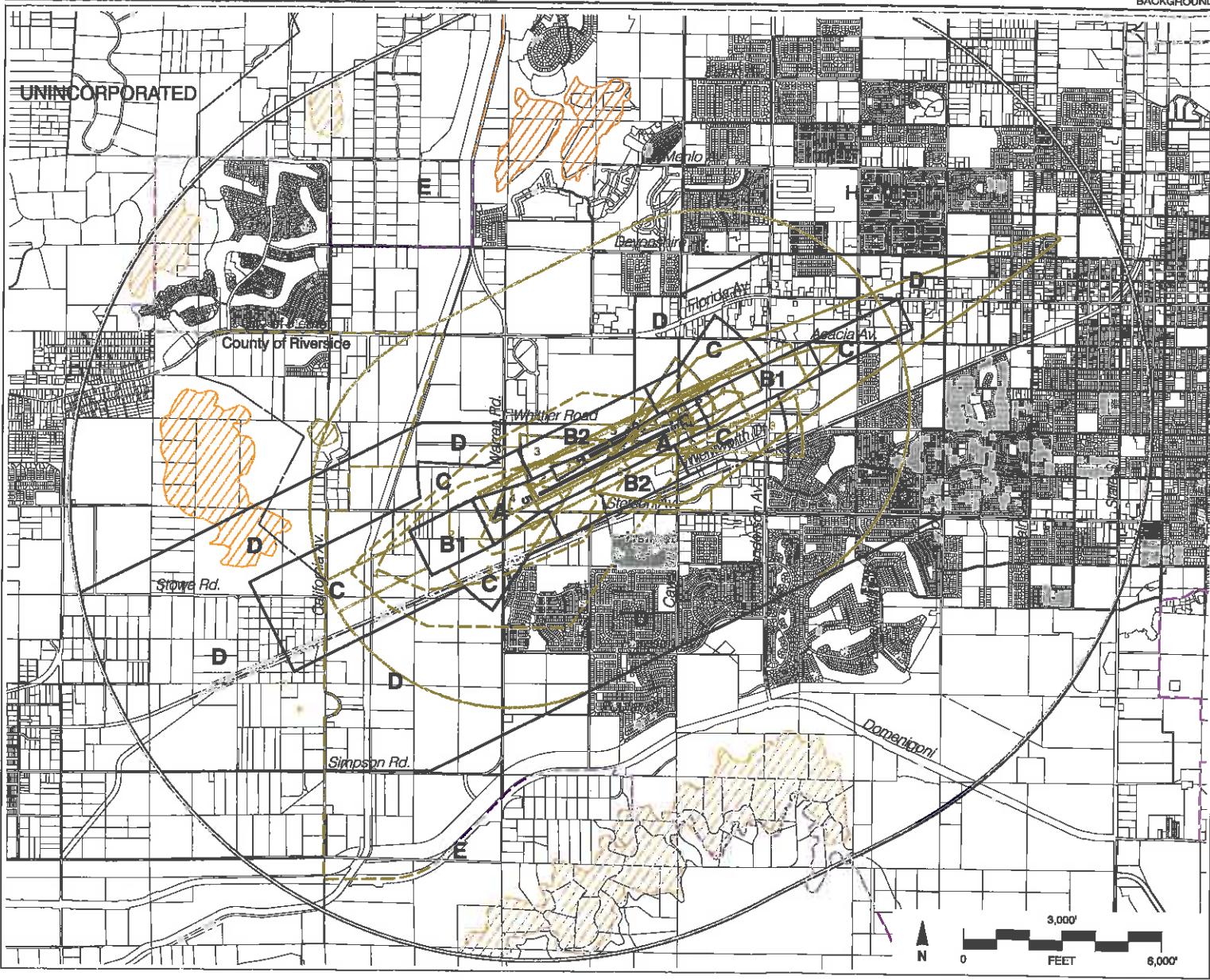
- Airport Property Line
- City Limits
- City Sphere of Influence
- Proposed Compatibility Zones
- Proposed Airport Influence Area

C:\Users\j70m\appdata\local\temp\AsaPublish_1305aa\HRT-compatibility 2016.dwg Nov 01, 2016 - 9:46am

**Riverside County
Airport Land Use Commission
Hemet-Ryan Airport
Land Use Compatibility Plan
(November 2016 Draft)**

**Exhibit HR-6
Compatibility Factors Map:
Noise and Overflight
Hemet-Ryan Airport**





Legend

- Safety and Airspace Compatibility Factors**
- Aircraft Departure Accident Risk Intensity Contours¹ (Shown only for Takeoffs to the West)
 - Aircraft Approach Accident Risk Intensity Contours¹ (Shown only for Landings from the East)
- Generic Safety Zones²**
- Medium Length General Aviation Runway (4000'-5999')
 - Low Activity General Aviation Runway (Less than 4000')
 - FAR Part 77 Conical Surface Limits
 - Terrain Penetration of FAR Part 77 Surfaces
- Boundary Lines**
- Airport Property Line
 - City Limits
 - City Sphere of Influence
 - Proposed Compatibility Zones
 - Proposed Airport Influence Area

1. Aircraft accident risk intensity contours are derived from accident location data in California Division of Aeronautics database. The contours represent relative intensities (highest concentrations) of near-airport accidents in 20% increments.
2. Source: California Airport Land Use Planning Handbook (2011).

**Riverside County
Airport Land Use Commission
Hemet-Ryan Airport
Land Use Compatibility Plan
(November 2016 Draft)**

Exhibit HR-7
**Compatibility Factors Map:
Safety and Airspace
Hemet-Ryan Airport**

C:\Users\67\OneDrive\OneDrive\Projects\HR\Compatibility 2016.dwg Nov 01, 2016 - 08:00am

AIRPORT SITE

- ▶ *Location*
 - › West-central Riverside County
 - › 3 miles west of Hemet city center
- ▶ *Topography*
 - › Situated in southern end of San Jacinto Valley; valley floor elevations 1,500–1,600 feet MSL
 - › Base of San Jacinto Mountains 10 miles east; Mt. San Jacinto peak (elevation 10,804 feet) 20 miles east
 - › Lower nearby hills including: Lakeview Mountains (max. elev. 2,649 ft.) to northwest; Double Butte (elev. 2,574 ft.) to west; Domenigoni Mountains to south; Santa Rosa Hills (max. elev. 3,343 ft.) to southeast
 - › Diamond Valley Lake 2.5 miles south

EXISTING AIRPORT AREA LAND USES

- ▶ *General Character*
 - › On western edge of Hemet urbanized area
 - › Farmland and clusters of rural residential to northwest and southwest
- ▶ *Runway Approaches*
 - › Southwest (Rwy 5): Road (1,200± feet from runway end); agricultural lands beyond
 - › Northeast (Rwy 23): Vacant land to 1± mile along centerline; commercial and industrial uses to each side
- ▶ *Traffic Pattern*
 - › North: Mobile home park and auto mall adjacent to airport; shopping center, residential, some vacant land beyond
 - › South: New residential subdivisions south and southeast; undeveloped to southwest

ESTABLISHED COMPATIBILITY MEASURES

- ▶ *Riverside County General Plan*
 - › "Prohibit new residential uses, except single-family dwellings on legal residential lots of record, within airports' 60 dB CNEL contours of any currently operating public-use or military airport ..." (Policy N 7.3)
 - › "Review all proposed projects and require consistency with any applicable compatibility plan ..." (LU 14.2)
 - › "Ensure that no structures or activities encroach upon or adversely affect the use of navigable airspace (LU 14.7)
 - › Submit proposed actions and projects to ALUC as required by state law (Policy LU 1.8); other actions may be submitted on voluntary, advisory basis (LU 14.8)

AIRPORT ENVIRONS LAND USE JURISDICTIONS

- ▶ *County of Riverside*
 - › Western and southern portions of airport environs in unincorporated county jurisdiction
- ▶ *City of Hemet*
 - › Entire airport property and most of airport environs within city limits
 - › Sphere of influence extends 1+ miles west and 3+ miles south of airport
- ▶ *City of San Jacinto*
 - › Nearest point to airport 2½ miles north (encompasses northern edge of airport FAR Part 77 airspace area)

STATUS OF COMMUNITY PLANS

- ▶ *County of Riverside*
 - › General Plan adopted by Board of Supervisors October 2003
 - › Harvest Valley /Winchester and San Jacinto Area Plans adopted October 2003, amended October 2011
- ▶ *City of Hemet*
 - › General Plan adopted January 2012
 - › Various Specific Plans cover areas north, south, and east of airport adopted from 1988 to date

PLANNED AIRPORT AREA LAND USES

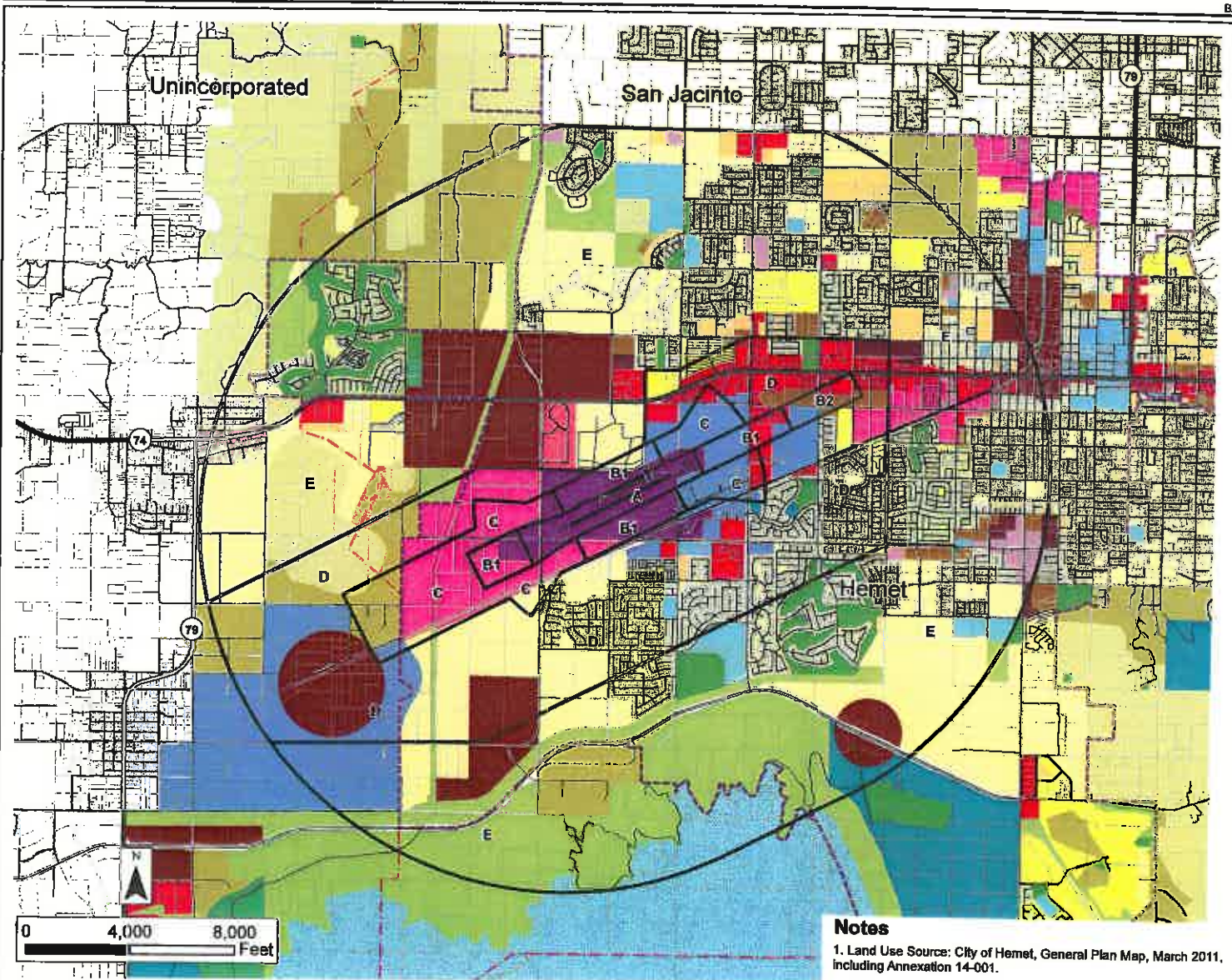
- ▶ *Riverside County*
 - › Mostly Estate Residential (2-acre minimum parcels) within 1± mile of runway end; low- and medium-density residential beyond
 - › State Route 79 realignment proposed west of airport; various alternatives under study
- ▶ *City of Hemet*
 - › Additional commercial and mixed-use development along Florida Avenue (St. Rte 74)
 - › Additional residential subdivisions north, south, and southwest of airport plus infill to east
 - › Close-in runway approaches planned for industrial uses to west, business park to east; low-density residential beyond 1.3 miles west; business park and mixed uses 1.3 miles southwest at future Metrolink station
- ▶ *City of Hemet General Plan (2012)*
 - › "Ensure that legislative land use decisions within airport influence area are consistent with the Airport Land Use Plan" (LU-10.1)
 - › "... ensure appropriate land use compatibility within airport safety zones" utilizing Hemet Airport Land Use Plan and Caltrans Airport Land Use Planning Handbook (LU-10.2)
 - › Use Interim Airport Overlay as basis for referring actions to ALUC for review until new Compatibility Plan is adopted (LU-10.4)
 - › "The City of Hemet has traditionally supported the Hemet-Ryan Airport ... but ... airport expansion should not be detrimental to the existing community and the necessary provision of surrounding circulation and infrastructure systems" (Circulation Section 4.2.7)

Exhibit HR-8

Airport Environs Information

Hemet-Ryan Airport

This page intentionally blank



- Legend**
- City Limits
 - City Sphere of Influence
 - Proposed Compatibility Zones
- City of Hemet General Plan Land Use**
- Rural Residential (0.0-2.0 du/ac)
 - Rural Residential - 2.5
 - Rural Residential - 5ac
 - Hillside Residential (0.0-0.5 du/ac)
 - Hillside Residential 10
 - Low Density Residential (2.1-5.0 du/ac)
 - Low Medium Density Residential (5.1-8.0 du/ac)
 - Medium Density Residential (8.1-18.0 du/ac)
 - High Density Residential (18.1-30.0 du/ac)
 - Very High Density Residential (30.1-45.0 du/ac)
 - Neighborhood Commercial (FAR 0.35)
 - Community Commercial (FAR 0.40)
 - Regional Commercial (FAR 0.50)
 - Industrial (FAR 0.45)
 - Airport
 - Mixed Use (Varies)
 - Business Park
 - Office Professional (FAR 2.0)
 - Open space
 - Quasi-Public/Cultural
 - ROW
 - Park/Recreation
 - Public Facilities
 - School
 - Agriculture
 - dvl

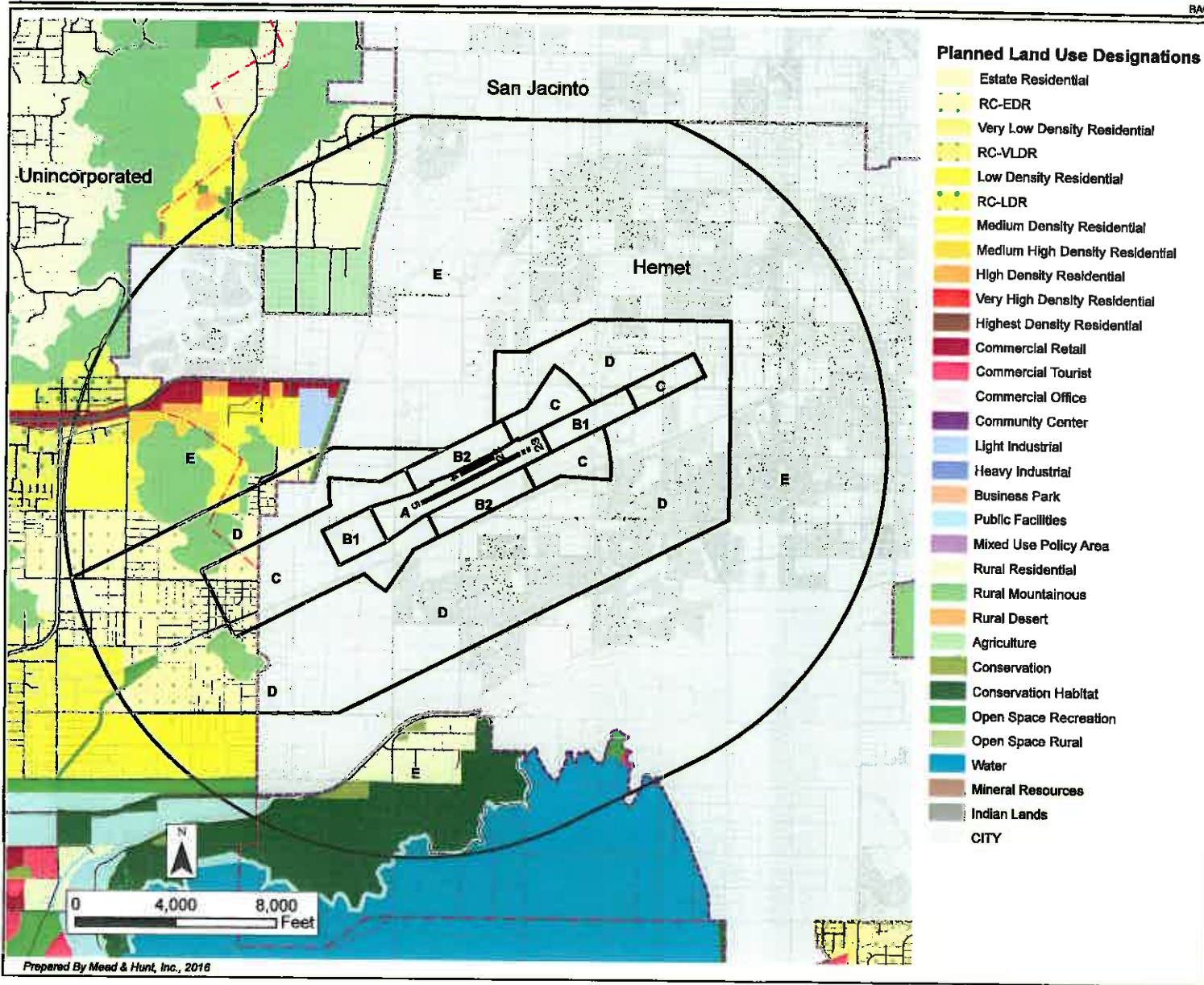
Riverside County
Airport Land Use Commission
Hemet-Ryan Airport
Land Use Compatibility Plan
(November 2016 Draft)

Notes

1. Land Use Source: City of Hemet, General Plan Map, March 2011, including Annexation 14-001.

Exhibit HR-9

Planned Land Uses:
City of Hemet



Planned Land Use Designations

- Estate Residential
- RC-EDR
- Very Low Density Residential
- RC-VLDR
- Low Density Residential
- RC-LDR
- Medium Density Residential
- Medium High Density Residential
- High Density Residential
- Very High Density Residential
- Highest Density Residential
- Commercial Retail
- Commercial Tourist
- Commercial Office
- Community Center
- Light Industrial
- Heavy Industrial
- Business Park
- Public Facilities
- Mixed Use Policy Area
- Rural Residential
- Rural Mountainous
- Rural Desert
- Agriculture
- Conservation
- Conservation Habitat
- Open Space Recreation
- Open Space Rural
- Water
- Mineral Resources
- Indian Lands
- CITY

Legend

- City Limits
- City Sphere of Influence
- Proposed Compatibility Zones

Notes

1. Land Use Source: County of Riverside, RCLMA GIS, 2013. The General Plan provides new land use designations for all parcels in the unincorporated area of Riverside County as adopted October 7, 2003, updated through January 2012.

**Riverside County
Airport Land Use Commission**

**Hemet-Ryan Airport
Land Use Compatibility Plan**
(November 2016 Draft)

Exhibit HR-10

**Planned Land Uses:
County of Riverside**

Prepared By Meed & Hunt, Inc., 2016

THE INITIAL STUDY AND NEGATIVE DECLARATION (CEQA DOCUMENT)

**INITIAL STUDY AND NEGATIVE DECLARATION:
AIRPORT LAND USE COMPATIBILITY PLAN
FOR HEMET-RYAN AIRPORT**

Prepared for:
Riverside County
Airport Land Use Commission
Riverside County Administrative Center
4080 Lemon Street, 14th Floor
Riverside, CA 92501

Prepared by:
Commission Staff
Riverside County Administrative Center
4080 Lemon Street, 14th Floor
Riverside, CA 92501

November 2016

TABLE OF CONTENTS

Page

[Proposed] Negative Declaration	1
Environmental Checklist: Initial Study	
Environmental Assessment	2
Environmental Factors Potentially Affected	14
Environmental Determination	15
Environmental Issues Assessment.....	16
Evaluation of Environmental Impacts	16
Aesthetics	18
Agriculture and Forest Resources	20
Air Quality	22
Biological Resources	24
Cultural Resources	27
Geology and Soils	29
Greenhouse Gas Emissions	31
Hazards and Hazardous Materials	33
Hydrology and Water Quality.....	36
Land Use and Planning	39
Mineral Resources	46
Noise	47
Population and Housing.....	51
Public Services.....	57
Recreation	59
Transportation/Traffic.....	60
Utilities and Service Systems.....	63
Mandatory Findings of Significance.....	65
Earlier Analyses	67
Sources	67

List of Figures

Figure 1 Airport Influence Area Comparison6
Figure 2 Compatibility Map.....7
Figure 3 Existing Land Use Designations8

List of Tables

Table 1 Anticipated Build-Out in the City of Hemet Pursuant to its General Plan42
Table 2 Potential Non-Residential Displacement Under the Compatibility Plan.....43
Table 3 Potential Residential Displacement Under the Compatibility Plan54

Appendix A Proposed Hemet-Ryan Airport Land Use Compatibility Plan..... A-1

[PROPOSED] NEGATIVE DECLARATION

Project Name: 2016 Hemet-Ryan Airport Land Use Compatibility Plan ("Compatibility Plan").

Lead Agency/Project Proponent: Riverside County Airport Land Use Commission ("Commission").

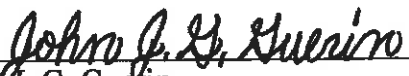
Brief Project Description: The proposed project is the Commission's adoption of a Compatibility Plan, which includes "Additional Compatibility Policies" specifically tailored to the land use environs of Hemet-Ryan Airport and revised Airport Influence Area ("AIA") boundaries. The revised AIA includes the area in which noise, safety, airspace protection, and/or overflight concerns may significantly affect land uses or necessitate restrictions on those uses, as determined by the Commission. Accordingly, the Compatibility Plan includes policies for determining whether a proposed development project, lying within the AIA, is consistent with the Compatibility Plan and the objectives set forth in the State Aeronautics Act, which include ensuring the continued operation of public-use airports (such as Hemet-Ryan Airport), while simultaneously protecting the public's health, safety, and welfare. (See Pub. Util. Code, §§21670-21679.5.) If adopted, the new Compatibility Plan will replace the Hemet-Ryan Airport Comprehensive Airport Land Use Plan ("HRACALUP") adopted in 1992 and amended in 2009.

Project Location: Hemet-Ryan Airport is located in the City of Hemet, southerly of Acacia and Florida Avenues, northerly of Stetson Avenue, easterly of Warren Road, and westerly of Sanderson Avenue. The revised AIA includes land around the Airport, within the City of Hemet and unincorporated Riverside County, and extends approximately 14,200 feet (2.69 miles) beyond the runway ends illustrated on the Airport Layout Plan (dated May 2013).

Initial Study: An Initial Study has been prepared in accordance with the California Environmental Quality Act ("CEQA;" Pub. Resources Code, §21000 *et seq.*) to ascertain whether the proposed project may have a significant effect on the environment. A copy of the Initial Study is attached to this proposed Negative Declaration and is incorporated by this reference.

Proposed Finding: The Commission finds that, on the basis of the whole record before it, there is no substantial evidence showing that the proposed project may have a significant effect on the environment. Additionally, there is no evidence that the project would have any effect on wildlife resources. This Initial Study/Negative Declaration reflects the Commission's independent judgment and analysis.

Date: November 2, 2016



John J. G. Guefin
Principal Planner
Riverside County Airport Land Use Commission

ENVIRONMENTAL CHECKLIST: INITIAL STUDY
Environmental Assessment Number ZAPEA02HR13

1. PROJECT TITLE

2016 Hemet-Ryan Airport Land Use Compatibility Plan (“Compatibility Plan”).

2. LEAD AGENCY NAME AND ADDRESS

Riverside County Airport Land Use Commission (“Commission”).

Riverside County Administrative Center
4080 Lemon Street, 14th Floor
Riverside, CA 92501

3. CONTACT PERSON AND INFORMATION

John J. G. Guerin, Principal Planner
(951) 955-0982
jguerin@rctlma.org

4. HEMET-RYAN AIRPORT DESCRIPTION AND PROJECT LOCATION

Hemet-Ryan Airport (hereinafter referred to as “the Airport”) is a County-owned, public-use airport located southerly of Acacia and Florida Avenues, easterly of Warren Road, northerly of Stetson Avenue, and westerly of Sanderson Avenue in the City of Hemet. The Airport comprises approximately 440 acres, and has two paved runways. Runway 5-23 is 4,315 feet in length and 100 feet in width. A secondary runway (Runway 4-22) restricted to sailplane and tow plane operations is 2,045 feet long and 25 feet wide.

The Airport serves the cities of Hemet, San Jacinto, and other nearby communities in the east-central portion of western Riverside County. A California Department of Forestry and Fire Protection (“CalFire”) base is located at the Airport, and the number of daily flights at the Airport reaches its peak during wildland fire events. Total annual aircraft operations were estimated at approximately 70,000 in the Draft Airport Master Plan issued in 2011. According to that document, single-engine piston aircraft accounted for approximately 72 percent of operations. The document indicated that there were 154 aircraft based at the Airport: 114 single-engine aircraft, 23 multi-engine piston aircraft, 8 helicopters, 4 turboprop aircraft, 4 sailplanes, and one jet.

Figure 1, Airport Influence Area Comparison, depicts the Airport Influence Area (“AIA”) boundaries for the proposed Compatibility Plan, which are different from those in the adopted Hemet-Ryan Airport Comprehensive Airport Land Use Plan (“HRACALUP”). The AIA is the area within which current or future airport-related noise, safety, airspace protection, and/or overflight factors may affect future land uses or necessitate restrictions on those uses.

The proposed boundaries of the AIA are defined by an arc consisting of a set of points located 14,200 feet from the runway ends depicted on the Airport Layout Plan (dated May 2013), except to the north where it does not extend northerly of Esplanade Avenue, the boundary line between the cities of Hemet and San Jacinto. The proposed Compatibility Plan is based on the Airport Layout Plan (dated May 2013), which identifies – as a future improvement to Hemet-Ryan Airport - a 500-foot easterly extension of Runway 5-23, with use of declared distances. All boundaries of Compatibility Zones A through D, however, are measured from the physical ends of the existing runways.

The area that would be subjected to development restrictions identified in the proposed Compatibility Plan, and the corresponding potential for displaced development, would be smaller than the area described above (i.e., the entire AIA is not subject to the potential for displaced development). The potentially affected area is limited to areas within 6,000 feet of the runway centerline and its straight-line northeasterly and southwesterly extensions. This geographic area (i.e., the area subject to the potential for displaced development) includes all of the compatibility zones identified in **Figure 2, Compatibility Map**, but for Zone E; in other words, the potential for displaced development occurs within Zones A, B1, B2, C, and D..

The potentially affected area (i.e., Zones A, B1, B2, C, and D) includes all or portions of the following Riverside County Townships and Ranges: Township 5 South, Range 1 West, Sections 7, 8, 9, 16 through 21, and 30; and Township 5 South, Range 2 West, Sections 13, 14, and 22 through 27. This area also would include Assessor Parcel Numbers located in the following County of Riverside Assessor’s Books: 448, 454, 456, 460, 463, 464, and 465.

5. GENERAL PLAN/COMMUNITY PLAN DESIGNATIONS

The applicable General Plan for most of the potentially affected area is the City of Hemet General Plan. Nonresidential land use designations in the area include: Community Commercial, Business Park, Industrial, Office Professional, Airport, Neighborhood Commercial, Regional Commercial, Quasi Public/Cultural, Park/Recreation, and Open Space. Residential designations run the gamut from Low Density Residential through Low Medium, Medium, and High Density Residential, culminating in the Very High Density Residential designation.

Unincorporated lands in the potentially affected area (within Zones C and D only) are subject to the provisions of the Riverside County General Plan and are within the Harvest Valley/Winchester Area Plan boundary. The primary residential land use designation in this area is Rural Community: Estate Density Residential, with areas of Rural: Rural Mountainous where topographic conditions merit such a designation and a Rural: Rural Residential area southerly of Stowe Road and westerly of El Callado. There are three pockets of suburban density designations (Community Development: Medium Density Residential and Low Density Residential). These pockets are located: (a) northerly of a

straight-line westerly extension of Stetson Avenue and westerly of California Avenue; (b) northerly of Stowe Road and westerly of Richmond Road; and (c) southerly of Grand Avenue and westerly of the southerly extension of El Callado. An area slightly greater than 100 acres southerly of Stowe Road and easterly of El Callado is designated Business Park.

Figure 3, Existing Land Use Designations, depicts the land use designations from the City of Hemet General Plan and Riverside County General Plan for property located within the AIA.

Both the City of Hemet and the County of Riverside have received determinations that their General Plans are consistent with the HRACALUP, as adopted in 1992 and amended in 2009. Adoption of this proposed Compatibility Plan would require both jurisdictions to evaluate consistency of their General Plans with the new Compatibility Plan and amend their General Plans to provide for consistency, or otherwise follow the State Aeronautics Act's overrule provisions described further below.

6. RELEVANT BACKGROUND/HISTORY

In 1967, the California Legislature established requirements for the creation of airport land use commissions ("ALUCs") in each county containing a public airport, as codified in the State Aeronautics Act (Pub. Util. Code, §21670 *et seq.*). The legislative findings and declarations set forth in section 21670 of the Public Utilities Code declare that "[i]t is in the public interest to provide for the orderly development of each public use airport in this state and the area surrounding these airports so as to promote the overall goals and objectives of the California airport noise standards . . . and to prevent the creation of new noise and safety problems." (Pub. Util. Code, §21670, subd. (a)(1).)

In addition, section 21670 defines the purpose of the statutory scheme, which is "to protect public health, safety, and welfare by ensuring the orderly expansion of airports and the adoption of land use measures that minimize the public's exposure to excessive noise and safety hazards within areas around public airports to the extent that these areas are not already devoted to incompatible uses." (Pub. Util. Code, §21670, subd. (a)(2).) In order to accomplish this objective, ALUCs are vested with certain powers and duties, which include:

- (a) Assisting local agencies in ensuring compatible land uses in the vicinity of airports to the extent that the land is not already devoted to incompatible uses;
- (b) Coordinating planning at the state, regional, and local levels so as to provide for the orderly development of air transportation, while at the same time protecting the public health, safety, and welfare;
- (c) Preparing and adopting airport land use compatibility plans; and

- (d) Reviewing the plans, regulations, and other actions of local agencies and airport operators for consistency with compatibility plans.

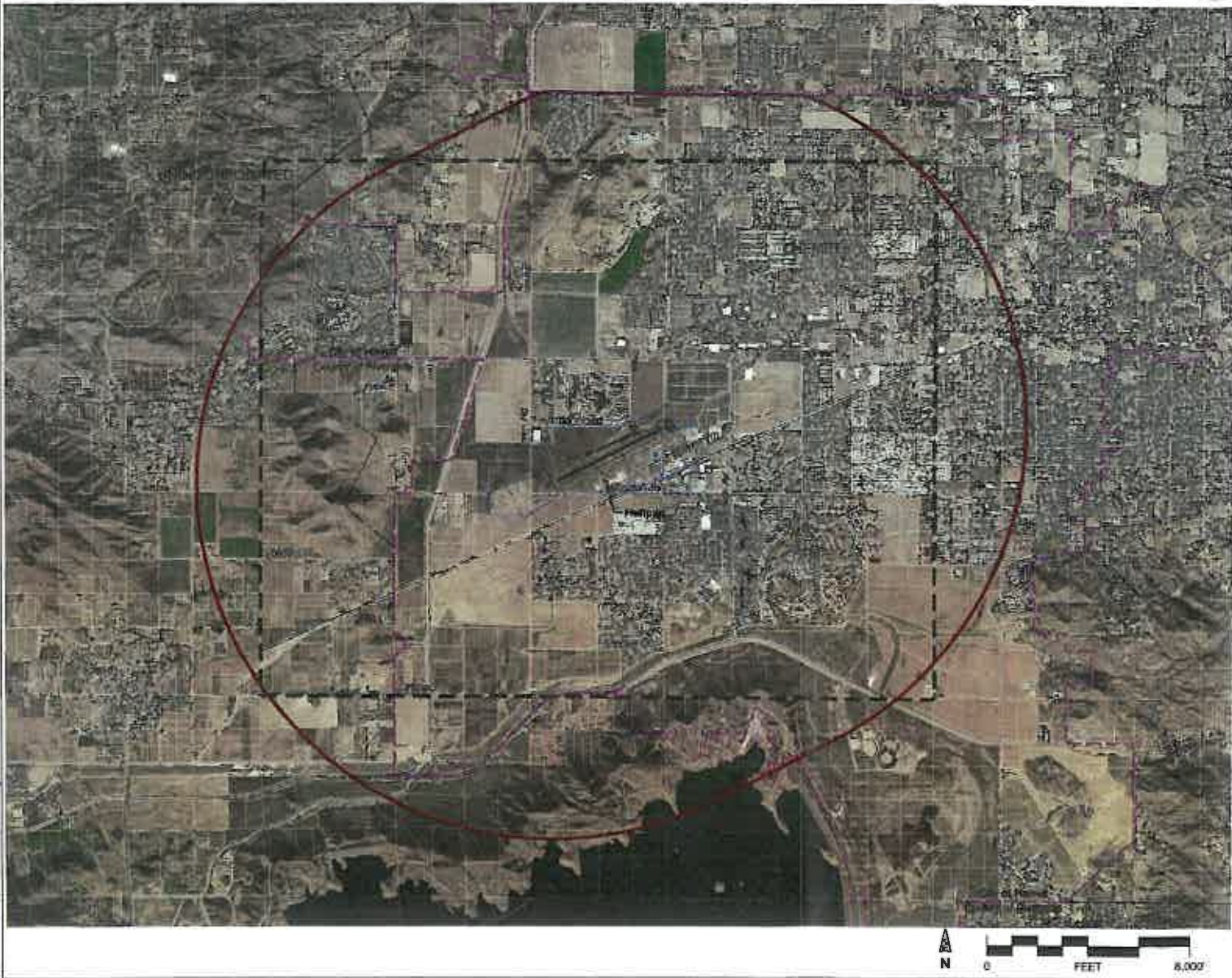
(Pub. Util. Code, §21674, subs. (a)-(d).)

ALUCs have no authority over existing land uses, even when such uses are inconsistent with compatibility plans (Pub. Util. Code, §21674, subd. (a)), or the operation of airports (Pub. Util. Code, §21674, subd. (e)).

An ALUC's adoption of, or amendment to, a compatibility plan triggers action by local agencies with jurisdiction over the geographic area encompassed by the AIA. Specifically, a local agency must submit its local land use plan(s) and/or regulation(s) (and any amendments thereto) to the ALUC in order for the ALUC to review the applicable documents and determine whether they are consistent with the compatibility plan. (Pub. Util. Code, §21676, subs. (a) and (b).) If the plan and/or regulation is considered inconsistent with the compatibility plan, the local agency's governing body may amend the documents to be consistent with the compatibility plan or overrule the ALUC's inconsistency determination after a hearing by a two-thirds vote. (*Ibid.*) In overruling the ALUC's determination, the local agency's governing body must make findings that its plan or regulation is consistent with the purposes of the State Aeronautics Act, as stated in Public Utilities Code section 21670. (*Ibid.*)

The Riverside County Airport Land Use Commission was first established in 1971, pursuant to the State Aeronautics Act (Pub. Util. Code, §21001 *et seq.*), for the purpose of ensuring the orderly expansion of airports and promulgating appropriate land use measures surrounding airports in Riverside County. (Pub. Util. Code, §21670, subd. (a)(2).) Pursuant to the directives of the State Aeronautics Act, the Commission drafted and adopted compatibility plans for public-use airports in Riverside County at various times from 1974 through present time. The Hemet-Ryan Airport Comprehensive Airport Land Use Plan was adopted in 1992 and is the only remaining plan for a public-use airport in Riverside County adopted prior to release of the 2002 *California Airport Land Use Planning Handbook*, which was subsequently updated in 2011.

C:\Users\BTOre\appdata\local\temp\1\1857-Compatibility_1312.dwg - Nov 08, 2016 - 3:40pm



Legend

Airport Influence Area

- Existing Airport Influence Area Boundary
- Proposed Airport Influence Area Boundary

Boundary Lines

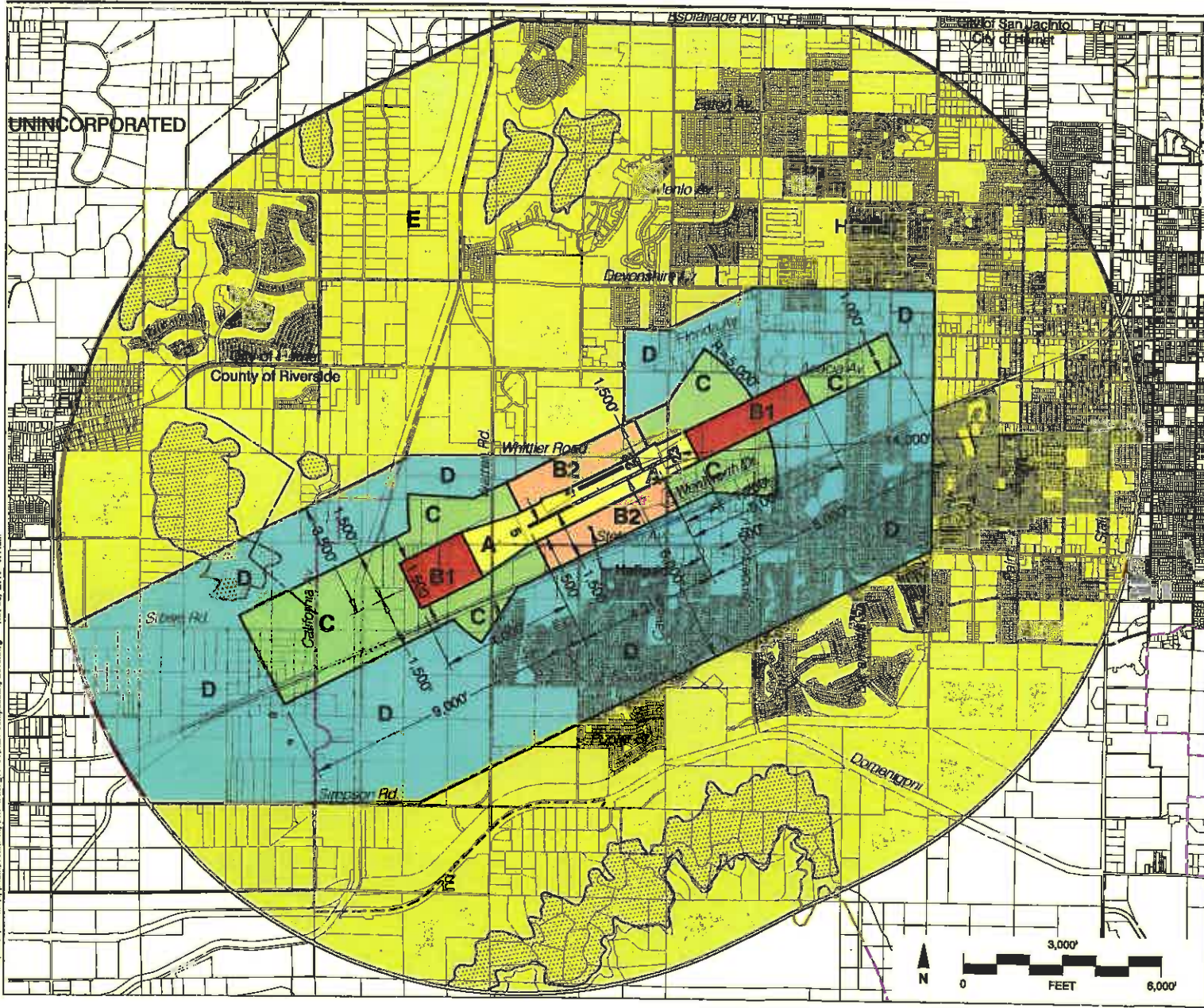
- Airport Priority Line
- City Sphere of Influence
- City Limits

Riverside County
Airport Land Use Commission
Hemet-Ryan Airport
Land Use Compatibility Plan
(November 2016 Draft)

Figure 1

Airport Influence Area Comparison
Hemet-Ryan Airport

C:\Users\6776\AppData\Local\Temp\Asp\Pub\eh_18154\HAF-compatibility 2016.dwg Nov 02, 2015 - 9:44am



Legend

- Compatibility Zones**
- Airport Influence Area Boundary
 - Zone A
 - Zone B1
 - Zone B2
 - Zone C
 - Zone D
 - Zone E
 - Height Review Overlay Zone

- Boundary Lines**
- Airport Property Line
 - City Limits
 - City Spheres of Influence

Note
 Airport Influence Area boundary measured from a point 200 feet beyond ends of proposed future 4,815 foot runway in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from ends and centerlines of existing 4,315 foot runway.

**Riverside County
 Airport Land Use Commission
 Hemet-Ryan Airport
 Land Use Compatibility Plan
 (November 2016 Draft)**

Figure 2

**Compatibility Map
 Hemet-Ryan Airport**

Existing Land Use Designations Figure 3

Hemet Land Use

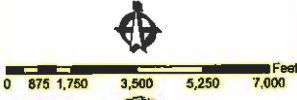
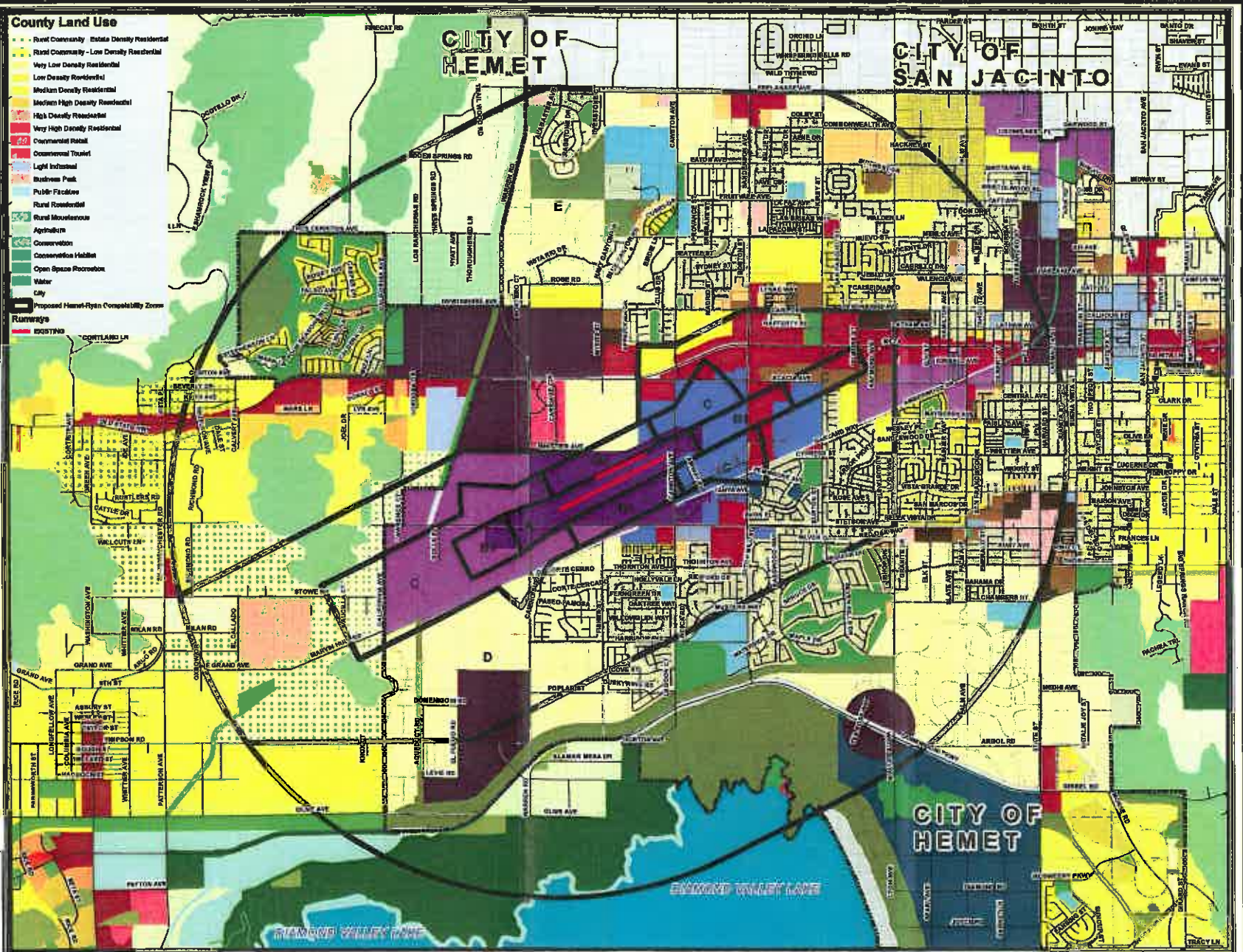
- Hillside Residential
- Rural Residential
- Low Density Residential
- Low Medium Density Residential
- Medium Density Residential
- High Density Residential
- Very High Density Residential
- Neighborhood Commercial
- Regional Commercial
- Community Commercial
- Mixed Use
- Office Professional
- Industrial
- Business Park
- Airport
- Public Facilities
- Quasi-Public/Cultural
- School
- Park
- Open Space
- Agriculture
- ROW

County Land Use

- Rural Community - Estate Density Residential
- Rural Community - Low Density Residential
- Very Low Density Residential
- Low Density Residential
- Medium Density Residential
- Medium High Density Residential
- High Density Residential
- Very High Density Residential
- Commercial Retail
- Commercial Tourist
- Light Industrial
- Business Park
- Public Facilities
- Rural Residential
- Rural Miscellaneous
- Agriculture
- Conservation
- Conservation Habitat
- Open Space Recreation
- Water
- City
- Proposed Hemet-Ryan Compatibility Zones

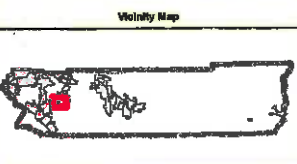
Runways

- EXISTING



Date: 10/6/2016

This map was prepared for the City of Hemet, Oregon. It is intended for informational purposes only and does not constitute a contract or warranty of any kind. The City of Hemet is not responsible for any errors or omissions on this map. The map is subject to change without notice. For more information, please contact the City of Hemet Planning Department.



7. CALIFORNIA AIRPORT LAND USE PLANNING HANDBOOK

To assist ALUCs in the performance of their duties, the California Department of Transportation, Division of Aeronautics (“Caltrans”), prepares the *California Airport Land Use Planning Handbook* (“Handbook”), the most recent edition of which was published in 2011. Pursuant to Public Utilities Code section 21674.7, subdivision (a), an ALUC that formulates, adopts, or amends a Compatibility Plan “shall be guided by” information in the Handbook.

The Handbook’s purpose is to support and amplify the statewide requirements for airport land use compatibility planning. Accordingly, the Handbook provides guidance for establishing the AIA and designating airport zones, and includes suggested methods for establishing safety compatibility zones for general aviation runways. The Handbook points out that the choice of safety criteria appropriate for a particular zone is largely a function of risk acceptability; the potentially severe consequences of aircraft accidents are the driving concern in setting safety compatibility criteria. For example, land uses that are determined to represent intolerable risks, given their proximity to an airport, usually must be prohibited; where risks of a particular land use are considered significant but tolerable, establishment of restrictions may reduce the risk to an acceptable level; and, uses that are intrinsically acceptable generally require no limitations.

8. PROJECT DESCRIPTION

The proposed project is the Commission’s adoption of an Airport Land Use Compatibility Plan that would be applicable to the environs of Hemet-Ryan Airport. The proposed Compatibility Plan includes “Additional Compatibility Policies” tailored specifically to the Airport’s land use environs and reflects the Commission’s efforts to develop a Compatibility Plan that fully satisfies the objectives of the State Aeronautics Act and addresses local concerns. The proposed Compatibility Plan, which includes revised AIA boundaries (as compared to those identified in the 1992 HRACALUP), is included as **Appendix A** to this document and is available at the Commission’s website: <http://www.rcaluc.org>. The proposed Compatibility Plan also is available for public review and inspection during ordinary business hours (Monday through Friday from 8:00 A.M. to 5:00 P.M.) at the Commission’s offices, located at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501.

The Compatibility Plan establishes policies for determining the consistency of future, proposed development projects within the Hemet-Ryan Airport AIA with the objectives set forth in the State Aeronautics Act (Pub. Util. Code, §§21670-21679.5). Those objectives call for the Commission to protect the public health, safety, and welfare by ensuring the orderly expansion of airports via the adoption of land use measures that minimize the public’s exposure to excessive noise and safety hazards within areas around public airports, to the extent that these areas are not already devoted to existing incompatible uses. (Pub. Util. Code, §21670, subd. (a)(2).)

The Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan identify the types and densities of permissible development within the Compatibility Zones of AIAs in the County of Riverside, except as modified by “Additional Compatibility Policies” included in Compatibility Plans adopted for the environs of specific airports.

The non-residential intensity restrictions incorporated in the Countywide Policies referenced above are more restrictive than the intensity levels recommended in the 2011 *California Airport Land Use Planning Handbook*. Therefore, the proposed Hemet-Ryan Airport Land Use Compatibility Plan includes two “Additional Compatibility Policies” that pertain to non-residential development.

The first of these policies relates to allowable intensities (persons per acre) of non-residential development. The Countywide Policies allow for average intensities of 25 persons per acre in Compatibility Zone B1, 75 persons per acre in Compatibility Zone C, and 100 persons per acre in Compatibility Zones B2 and D, with allowable single-acre maxima of 50 persons in Compatibility Zone B1, 150 persons in Compatibility Zone C, 200 persons in Compatibility Zone B2, and 300 persons in Compatibility Zone D. Additional Compatibility Policy 2.1 allows average intensities of 40 persons per acre in Compatibility Zone B1, 100 persons per acre in Compatibility Zones B2 and C, and 200 persons per acre in Compatibility Zone D, with allowable single-acre maxima of 80 persons in Compatibility Zone B1, 250 persons in Compatibility Zone C, 300 persons in Compatibility Zone B2, and 800 persons in Compatibility Zone D (prior to application of risk-reduction bonuses).

The second of these policies addresses the calculation of concentrations of people in retail sales establishments. Specifically, while the California Building Code allows the number of persons in ground floor mercantile establishments to be as high as one person per 30 square feet, this level is recognized as too high to be realistic. Based on subsequent studies and in light of assumptions utilized in more recently adopted Airport Land Use Compatibility Plans, Additional Compatibility Policy 2.2 proposes that retail sales and indoor display establishments (excluding restaurants and other uses identified separately from retail or mercantile uses) be evaluated as having an intensity of one person per 115 square feet.

The Countywide Policies also include residential density restrictions in Compatibility Zone D, a geographic area that largely coincides with the Traffic Pattern Zone, as discussed in the 2011 *California Airport Land Use Planning Handbook*. The Countywide Policies allow densities not exceeding one dwelling unit per five acres or densities of at least five dwelling units per net acre or greater within Compatibility Zone D, but not the intermediate residential densities greater than one dwelling unit per five acres, but less than five dwelling units per net acre. Here, Additional Compatibility Policy 2.3 allows densities up to one dwelling unit per 2½ acres and densities of 4.5 dwelling units per acre or greater. The Compatibility Plan would retain the prohibition

on densities greater than one dwelling unit per 2½ acres and less than 4.5 dwelling units per acre.

Additional Compatibility Policy 2.3 is proposed in light of the following facts:

- a. The HRACALUP limited residential density in Area II to one dwelling unit per 2½ acres due to significant safety concerns. The Harvest Valley/Winchester Area Plan includes a policy acknowledging this provision and limiting densities in the areas subject to the Area II density limitation.
- b. Much of the vacant land remaining in portions of the City of Hemet within Compatibility Zone D is designated Low Density Residential, which limits residential density to five dwelling units per acre.
- c. The airport operator has advised that very few noise complaints have been made regarding aircraft flying in the vicinity of Hemet-Ryan Airport in comparison to French Valley Airport, which also serves general aviation aircraft.

9. **DISPLACED DEVELOPMENT**

The phrase “potential for displaced development” is defined in this section to assist the Commission, local land use agencies, and all other interested parties in understanding how the potential for displaced development may arise in the context of formulating and adopting the proposed Compatibility Plan. The potential for displaced development results from placing certain land use density or intensity restrictions on future development within the AIA in order to ensure the continued operation and orderly expansion of Hemet-Ryan Airport, minimizing the public’s exposure to excessive noise and safety hazards within certain areas surrounding the Airport. Said restrictions may have the consequence, notwithstanding existing General Plan designations and zoning, of displacing future development to other areas outside or inside the AIA, such as Zone E where densities and intensities are not subject to substantial restrictions.

As defined, adoption of the proposed Compatibility Plan has the potential to displace development that would otherwise occur within the AIA.¹ That being said, the Additional Compatibility Policies included in the proposed Compatibility Plan lessen the extent of potential development displacement, as discussed further below. Nonetheless, by restricting development in the Airport’s environs, there is the potential for increased pressure for growth and development in other portions of the AIA and/or in areas outside the AIA. These indirect impacts could result in the construction and operation of residential and non-residential development in the surrounding environment. This development, if it occurs, may result in traffic and associated air quality and noise

¹ The Airport Influence Area is the area in which current or future airport-related noise, overflight, safety, and/or airspace protection factors may affect land uses or necessitate restrictions on those uses. It also constitutes the area within which certain land use actions are subject to Commission review. The AIA boundary for Hemet-Ryan Airport is depicted in **Figure 1 and Figure 2**, above.

impacts. Such development also could impact sensitive visual, biological, cultural, paleontological, historic, and other resources.

However, any such future development would be dependent on what the affected local land use jurisdictions (here, the City of Hemet and, to a lesser extent, Riverside County and nearby cities such as San Jacinto and Menifee) would permit. Moreover, any project-specific impacts necessarily would be considered in later environmental documents prepared in compliance with CEQA. As a result, there is no potential that such impacts would escape appropriate, further environmental review at the project-level. Although not determinative, the fact that potential shifts in population and future development will require further environmental analysis at the project level is an important and relevant consideration.

In addition, the Commission believes that by conducting an analysis of the environmental effects of population and development shifts in the region surrounding Hemet-Ryan Airport, the Commission will have informed the affected jurisdictions (in particular, the City of Hemet, Riverside County, and neighboring cities) of the potential for displaced development, and the associated consequences, before possible population and development pressure mounts in areas outside the Compatibility Plan's AIA.

After having disclosed the potential for displaced population/development due to adoption of the Compatibility Plan, and having analyzed and quantified that potential for displaced development (see *infra*, *Land Use and Planning* and *Population and Housing* sections), it is equally important to acknowledge that whether actual population and development shifts will, in fact, occur in surrounding areas in any particular case necessarily depends on a multitude of factors, including, but not limited to, the rate, timing, location, and extent of development, economic and market conditions, the nature and type of the project or projects, and the contemplated project-level impacts on the environment.

At this time, the Commission is unable to accurately forecast the actual effects such future shifts in population/development, if they do occur, will have on the physical environment. The Commission also finds that any attempt to forecast such eventualities, including predictions about the significance of any environmental effects, is impracticable, speculative, and potentially misleading at this time. It is impracticable because such impacts are too uncertain, and it is potentially misleading because the "science" of assessing impacts, and their significance, does not permit an accurate assessment at this time. Rather than speculate, the Commission has determined that the uncertainties justify its finding that population/development shifts, if they do occur, cannot reasonably be characterized as significant at this time and that if such shifts occur in the future, they will not escape project-level environmental review under CEQA. (See Cal. Code Regs., tit. 14, section 15145.)

In summary, the Commission finds, after a thorough investigation, that particular environmental impacts resulting from the potential for displaced population/development

are too speculative for any evaluation beyond this analysis and that found below, in the *Land Use and Planning* and *Population and Housing* sections; accordingly, the Commission's assessment is appropriately concluded.

10. PROJECT OBJECTIVES

The principal project objectives include the following: (i) to meet the California legislative mandate to prepare and adopt a Compatibility Plan for Hemet-Ryan Airport, pursuant to the requirements of the State Aeronautics Act and the guidelines of the *California Airport Land Use Planning Handbook*; and (ii) to adopt a Compatibility Plan for Hemet-Ryan Airport that will provide for the orderly growth of the Airport and the area surrounding the Airport, and which will safeguard the public health, safety, and general welfare of the inhabitants within the vicinity of the Airport and the public in general, consistent with the requirements of the State Aeronautics Act. (Pub. Util. Code, §§21670.3; 21675.)

11. SURROUNDING LAND USES AND SETTING

Hemet-Ryan Airport is located in the City of Hemet, to the west of the historic Downtown Hemet area. As shown in **Figure 1**, most of the area immediately adjacent to the Airport is either vacant or developed at suburban densities/intensities. **Figure 3** depicts existing City of Hemet General Plan land use designations within the portions of the AIA in the City and County General Plan (Harvest Valley/Winchester Area Plan) land use designations in the portion of the AIA in unincorporated Riverside County.

12. OTHER PUBLIC AGENCIES WHOSE APPROVAL IS REQUIRED

Input and comments from other agencies, including the City of Hemet and the County of Riverside, will be considered by the Commission. And, the Commission seeks to establish and maintain collaborative working relationships with the local agencies that ultimately will provide for implementation of the proposed Compatibility Plan's policies. However, approval of the affected local agencies is not a statutory requirement; state law empowers ALUCs to adopt Compatibility Plans without the approval of the affected local agencies.

A copy of the proposed Compatibility Plan shall be submitted to the California Department of Transportation, Division of Aeronautics, pursuant to Public Utilities Code section 21675, subdivision (d).

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED

None of the environmental topics below would be potentially significantly affected by the proposed project, because none of the impacts are considered a “Potentially Significant Impact” as indicated by the checklist on the following pages.

- | | |
|---|---|
| <input type="checkbox"/> Aesthetics | <input type="checkbox"/> Greenhouse Gas Emissions |
| <input type="checkbox"/> Agriculture and Forest Resources | <input type="checkbox"/> Hazards and Hazardous Materials |
| <input type="checkbox"/> Air Quality | <input type="checkbox"/> Hydrology and Water Quality |
| <input type="checkbox"/> Biological Resources | <input type="checkbox"/> Land Use and Planning |
| <input type="checkbox"/> Cultural Resources | <input type="checkbox"/> Mineral Resources |
| <input type="checkbox"/> Geology and Soils | <input type="checkbox"/> Noise |
| <input type="checkbox"/> Population and Housing | <input type="checkbox"/> Transportation/Traffic |
| <input type="checkbox"/> Public Services | <input type="checkbox"/> Utilities and Service Systems |
| <input type="checkbox"/> Recreation | <input type="checkbox"/> Mandatory Findings of Significance |

ENVIRONMENTAL DETERMINATION

On the basis of this evaluation:

- I find that the proposed project **COULD NOT** have a significant effect on the environment, and a **NEGATIVE DECLARATION** will be prepared.
- I find that the proposed project could not have a significant effect on the environment, but, nonetheless, one or more of the mitigation measures described in this document have been recommended for inclusion into the proposed project. Therefore, a **MITIGATED NEGATIVE DECLARATION** will be prepared.
- I find that the proposed project **MAY** have a significant effect on the environment, and an **ENVIRONMENTAL IMPACT REPORT** is required.
- I find that the proposed project **MAY** have a significant effect(s) on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets, if the effect is a “potentially significant impact” or “potentially significant unless mitigated.” An **ENVIRONMENTAL IMPACT REPORT** is required, but it must analyze only the effects that remain to be addressed.
- I find that although the proposed project could have a significant effect on the environment, there **WILL NOT** be a significant effect in this case because all potentially significant effects (a) have been analyzed adequately in an earlier EIR pursuant to applicable standards; and (b) have been avoided or mitigated pursuant to that earlier EIR, including revisions or mitigation measures that are imposed upon the proposed project.

By: John J. G. Guerin
John J. G. Guerin, Principal Planner
Riverside County Airport Land Use Commission

ENVIRONMENTAL ISSUES ASSESSMENT

In accordance with CEQA, this Initial Study analyzes whether any potential significant impacts upon the environment would result from the proposed project. In accordance with section 15063 of the CEQA Guidelines, this Initial Study is a preliminary analysis prepared by the lead agency, the Riverside County Airport Land Use Commission, in consultation with other jurisdictional agencies, to determine whether a Negative Declaration, Mitigated Negative Declaration, or an Environmental Impact Report is required for the proposed project. The purpose of this Initial Study is to inform the decision-makers, affected agencies, and the public of potential environmental impacts associated with the implementation of the proposed project.

EVALUATION OF ENVIRONMENTAL IMPACTS

1. A brief explanation is required for all answers except “No Impact” answers that are adequately supported by the information sources a lead agency cites. A “No Impact” answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (*e.g.*, the project falls outside a fault rupture zone). A “No Impact” answer should be explained where it is based on project-specific factors as well as general standards (*e.g.*, the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).
2. All answers must take into account the whole of the action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
3. Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. “Potentially Significant Impact” is appropriate if there is substantial evidence that an effect may be significant. If there are one or more “Potentially Significant Impact” entries when the determination is made, an EIR is required.
4. “Negative Declaration: Less Than Significant With Mitigation Incorporated” applies where the incorporation of mitigation measures has reduced an effect from “Potentially Significant Impact” to a “Less than Significant Impact.” The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from “Earlier Analyses,” as described in (5) below, may be cross-referenced).

5. Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration. (Cal. Code Regs., tit. 14, §15063, subd. (c)(3)(D).) In this case, a brief discussion should identify the following:
 - (a) *Earlier Analysis Used.* Identify and state where the earlier analysis is available for review.
 - (b) *Impacts Adequately Addressed.* Identify the effects from the checklist that were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.
 - (c) *Mitigation Measures.* For effects that are “Less than Significant with Mitigation Measures Incorporated,” describe the mitigation measures that were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.
6. References to supporting information sources for potential impacts have been identified and incorporated into the analysis. A source list is provided following the checklist. Other sources used or individuals contacted have been cited in the respective discussions.
7. The following environmental analysis checklist is based on Appendix G of the CEQA Guidelines and was prepared to identify the potential environmental effects of the proposed project. After each environment topic is assessed, an analysis of the basis for the assessment is provided.
8. The explanation of each issue should identify:
 - (a) the significance criteria or threshold, if any, used to evaluate each question; and
 - (b) the mitigation measure identified, if any, to reduce the impact to a less-than-significant level.

ENVIRONMENTAL ANALYSIS CHECKLIST

AESTHETICS

Issues	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
(a) Have a substantial adverse effect on a scenic vista?			X	
(b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?			X	
(c) Substantially degrade the existing visual character or quality of the site and its surroundings?			X	
(d) Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?			X	

ANALYSIS:

Thresholds (a) - (d): The Compatibility Plan does not propose or entail any new development, construction, or physical changes to existing land uses or the environment. Therefore, the Compatibility Plan would not directly affect a scenic vista, damage scenic resources, degrade the existing visual character or quality of the site or its surroundings, or create a new source of light or glare; and, as such, the project would not directly impact the environment or result in any direct impacts to aesthetics. Further, the Compatibility Plan would not increase the levels of development in the affected areas of the City of Hemet and unincorporated areas of the County of Riverside above those projected for this area in the City of Hemet General Plan and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), the environmental effects of which were already adequately analyzed in the certified City of Hemet General Plan EIR and the County General Plan EIR.

The Compatibility Plan may indirectly influence future land use development in the vicinity of Hemet-Ryan Airport by facilitating development at some locations and constraining development at other locations. However, any potential indirect effect that may arise is uncertain from a timing and location standpoint, and it is speculative to anticipate the specific characteristics of any development or the types of aesthetic impacts that would be associated with it. One possibility is that land uses in much of the Airport environs would remain unchanged when compared to existing conditions. Another possibility is that the Compatibility Plan may indirectly result in shifting future housing, facilities, and infrastructure to other locations designated or zoned to allow for such uses. Because such shifts, if they do occur,

cannot be accurately predicted, particularly as to rate, timing, location, and extent, it is not considered reasonable to conclude that such shifts, if any, will be significant. Accordingly, absent information to the contrary, any such shifts are reasonably considered less than significant.

In addition, nothing in the Compatibility Plan would result in indirect impacts like, for example, the construction of housing or other uses, or the extension of roads or other infrastructure, all of which would necessitate an analysis of potentially significant aesthetic impacts. Moreover, the Compatibility Plan would not result in the displacement of existing housing, structures, or people, necessitating the construction of replacement housing, facilities, or infrastructure, which may result in potentially significant aesthetic impacts. Therefore, any further analysis necessarily will occur if future shifts in development or construction result in a “project” subject to further CEQA compliance. All such projects will be required to comply with CEQA, necessitating preparation of a negative declaration, a mitigated negative declaration, or an EIR. This project-level environmental documentation ensures that if future shifts in development/construction indirectly result from the Compatibility Plan, these subsequent actions will not evade further project-level environmental review.

Further, as noted above, because implementation of the Compatibility Plan would not increase the levels of development in the AIA above those projected in the City of Hemet General Plan (including its constituent Specific Plans) and the Riverside County General Plan (specifically the Harvest Valley/Winchester Area Plan), and because the environmental effects of such development were already adequately analyzed at the General Plan level, there is no substantial evidence that adoption of the Compatibility Plan would result in any significant indirect impacts on aesthetics. Moreover, with respect to the creation of new sources of light or glare, which would adversely affect day or nighttime views in the area, any proposed development within the area would be subject to standard conditions from the Commission or the City restricting lighting that could interfere with the safe operation of aircraft.

(Sources: City of Hemet 2030 General Plan, as adopted January 24, 2012 (“City General Plan”); Riverside County General Plan, as adopted October 7, 2003 (“County General Plan”); City of Hemet General Plan 2030 Final Environmental Impact Report, dated January 2012 (State Clearinghouse No. 2010061088) (“Hemet EIR”); Riverside County General Plan Final Program Environmental Impact Report No. 441, dated October 2003 (State Clearinghouse No. 2002051143) (“County EIR No. 441”).

PLAN MITIGATION: No mitigation necessary.

OTHER APPLICABLE/ADOPTED MITIGATION MEASURES: Pursuant to Policy PS-4.8 of the Public Safety Element of the City of Hemet General Plan and Note 1 of Table 4 of the Harvest Valley/Winchester Area Plan (Riverside County General Plan), the following uses are prohibited in the Hemet-Ryan AIA:

- (a) Any use that would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight

final approach toward a landing at Hemet-Ryan Airport, other than a navigational signal light or visual approach slope indicator approved by the Federal Aviation Administration.

- (b) Any use that would cause sunlight to be reflected towards an aircraft engaged in initial straight climb following takeoff or towards an aircraft engaged in a straight final approach toward a landing at Hemet-Ryan Airport.
- (c) Any use that would generate smoke or water vapor, could attract large concentrations of birds, or may otherwise affect safe air navigation within the area.

AGRICULTURE AND FOREST RESOURCES

Issues	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
(a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?			X	
(b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?			X	
(c) Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Govt. Code section 51104(g))?			X	
(d) Result in the loss of forest land or conversion of forest land to non-forest use?			X	
(e) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?			X	

ANALYSIS:

Thresholds (a) - (e): The Compatibility Plan does not propose or entail any new development, construction, or physical changes to existing land uses or the environment. Therefore, the proposed Compatibility Plan would not: (a) directly convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (collectively, "Farmland") to a non-agricultural use; (b) conflict with existing zoning for agricultural use, or a Williamson Act contract; (c) conflict with existing zoning for, or cause rezoning of, forest land, timberland, or land zoned Timberland Production; (d) result in the loss of forest land or conversion of forest land to non-forest use; or (e) involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland to non-agricultural use or forest land to non-forest use. Further, the Compatibility Plan would not increase the levels of development in the affected areas of the City of Hemet and unincorporated areas of the County of Riverside above those projected for these areas in the City of Hemet General Plan and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), the environmental effects of which were already adequately analyzed in the certified Hemet EIR and County EIR No. 441. Indeed, to the extent that the proposed Compatibility Plan would restrict future development of residential and non-residential land uses in certain areas, the direct effect of the proposed Compatibility Plan, if any, would be to reduce the conversion of farmland to non-agricultural uses and the conversion of forest land to non-forest uses.

The proposed Compatibility Plan may indirectly influence future land use development in the vicinity of Hemet-Ryan Airport by facilitating development at some locations and constraining development at other locations. However, any potential indirect effect that may arise is uncertain from a timing and location standpoint, and it is speculative to anticipate the specific characteristics of any development or the types of impacts to agricultural and forest resources that would be associated with it. One possibility is that land uses in much of the Airport's environs would remain unchanged when compared to existing conditions. Another possibility is that the Compatibility Plan may indirectly result in shifting future housing, facilities, and infrastructure to other locations designated or zoned to allow for such uses. Because such shifts, if they do occur, cannot be accurately predicted, particularly as to rate, timing, location, and extent, it is not considered reasonable to conclude that such shifts, if any, will be significant. Accordingly, absent information to the contrary, any such shifts are reasonably considered less than significant.

In addition, nothing in the proposed Compatibility Plan would result in indirect impacts like, for example, the construction of housing or other uses, or the extension of roads or other infrastructure, all of which would necessitate an analysis of potentially significant impacts to agricultural and forest resources. Moreover, the Compatibility Plan would not result in the displacement of existing housing, structures, or people, necessitating the construction of replacement housing, facilities, or infrastructure, which may result in potentially significant impacts to agricultural and forest resources. Therefore, any further analysis necessarily will occur if future shifts in development or construction result in a "project" subject to further CEQA compliance. All such projects will be required to comply with CEQA, necessitating preparation of a negative declaration, a mitigated negative declaration, or an EIR. This project-level environmental documentation ensures that if future shifts in development/construction

indirectly result from the Compatibility Plan, these subsequent actions will not evade further project-level environmental review.

Further, as noted above, because implementation of the Compatibility Plan would not increase the levels of development in the AIA above those projected in the City of Hemet General Plan (including its constituent Specific Plans) and the Riverside County General Plan (specifically the Harvest Valley/Winchester Area Plan), and because the environmental effects of such development were already adequately analyzed at the General Plan level, there is no substantial evidence that adoption of the Compatibility Plan would result in any significant indirect impacts on agricultural and forest resources.

(Sources: City General Plan and related Hemet EIR; County General Plan and related County EIR No. 441.)

PLAN MITIGATION: No mitigation necessary.

OTHER APPLICABLE/ADOPTED MITIGATION MEASURES: None.

AIR QUALITY

Issues	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
(a) Conflict with or obstruct implementation of the applicable air quality plan?			X	
(b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation?			X	
(c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?			X	
(d) Expose sensitive receptors to substantial pollutant concentrations?			X	
(e) Create objectionable odors affecting a substantial number of people?			X	

ANALYSIS:

Thresholds (a) - (e): The Compatibility Plan does not propose or entail any new development, construction, or physical changes to existing land uses or the environment. Therefore, the Compatibility Plan would not conflict with or obstruct implementation of the applicable air quality plan; violate any air quality standard or contribute substantially to an existing or projected air quality violation; result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard; expose sensitive receptors to substantial pollutant concentrations; or, create objectionable odors affecting a substantial number of people. As such, the project would not directly impact the environment or result in any direct impacts to air quality. Further, the Compatibility Plan would not increase the levels of development in the affected areas of the City of Hemet and unincorporated areas of the County of Riverside above those projected for these areas in the City of Hemet General Plan and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), the environmental effects of which were already adequately analyzed in the certified Hemet EIR and County EIR No. 441.

The Compatibility Plan may indirectly influence future land use development in the vicinity of Hemet-Ryan Airport by facilitating development at some locations and constraining development at other locations. However, any potential indirect effect that may arise is uncertain from a timing and location standpoint, and it is speculative to anticipate the specific characteristics of any development or the types of air quality impacts that would be associated with it. One possibility is that land uses in much of the Airport's environs would remain unchanged when compared to existing conditions. Another possibility is that the Compatibility Plan may indirectly result in shifting future housing, facilities, and infrastructure to other locations designated or zoned to allow for such uses. Because such shifts, if they do occur, cannot be accurately predicted, particularly as to rate, timing, location, and extent, it is not considered reasonable to conclude that such shifts, if any, will be significant. Accordingly, absent information to the contrary, any such shifts are reasonably considered less than significant.

In addition, nothing in the Compatibility Plan would result in indirect impacts like, for example, the construction of housing or other uses, or the extension of roads or other infrastructure, all of which would necessitate an analysis of potentially significant air quality impacts. Moreover, the Compatibility Plan would not result in the displacement of existing housing, structures, or people, thereby necessitating the construction of replacement housing, facilities, or infrastructure, which may result in potentially significant air quality impacts. Therefore, any further analysis necessarily will occur if future shifts in development or construction result in a "project" subject to further CEQA compliance. All such projects will be required to comply with CEQA, necessitating preparation of a negative declaration, a mitigated negative declaration, or an EIR. This project-level environmental documentation ensures that if future shifts in development/construction indirectly result from the Compatibility Plan, these subsequent actions will not evade further project-level environmental review.

Further, as noted above, because implementation of the Compatibility Plan would not increase the levels of development in the AIA above those projected in the City of Hemet General Plan

(including its constituent Specific Plans) and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), and because the environmental effects of such development were already adequately analyzed at the General Plan level, there is no substantial evidence that adoption of the Compatibility Plan would result in any significant indirect impacts on air quality.

(Sources: City General Plan and related Hemet EIR; County General Plan and related County EIR No. 441.)

PLAN MITIGATION: No mitigation necessary.

OTHER APPLICABLE/ADOPTED MITIGATION MEASURES: The City of Hemet already has committed to mitigate development-related impacts to air quality through compliance with Air Quality Mitigation Measures 4.3-1a, 4.3-1b, 4.3-1c, 4.3-1d, 4.3-1e, 4.3-4a, and 4.3-4b, as specified in the Hemet EIR. Development within the unincorporated areas of Riverside County shall comply with Mitigation Measures 4.5.1A, 4.5.1B, and 4.5.1C, as specified in EIR No. 441.

BIOLOGICAL RESOURCES

Issues	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
<i>Would the project:</i>				
(a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?			X	
(b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulation, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?			X	
(c) Have a substantial adverse effect on federally-protected wetlands, as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, <i>etc.</i>) through direct removal, filling, hydrological interruption, or other means?			X	
(d) Interfere substantially with the movement of any native resident or migratory fish or wildlife			X	

BIOLOGICAL RESOURCES

Issues	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?				
(e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?			X	
(f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?			X	

ANALYSIS:

Thresholds (a) - (f): The Compatibility Plan does not propose or entail any new development, construction, or physical changes to existing land uses or the environment. Therefore, the Compatibility Plan would not impact biological resources or their habitat, or conflict with applicable policies protecting biological resources or an adopted or approved habitat conservation plan, and, as such, would not directly impact the environment or result in any direct impacts to biological resources. Further, the Compatibility Plan would not increase the levels of development in the affected areas of the City of Hemet and unincorporated areas of the County of Riverside above those projected for these areas in the City of Hemet General Plan and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), the environmental effects of which were already adequately analyzed in the certified Hemet EIR and County EIR No. 441.

The Compatibility Plan may indirectly influence future land use development in the vicinity of Hemet-Ryan Airport by facilitating development at some locations and constraining development at other locations. However, any potential indirect effect that may arise is uncertain from a timing and location standpoint, and it is speculative to anticipate the specific characteristics of any development or the types of impacts to biological resources that would be associated with it. One possibility is that land uses in much of the Airport's environs would remain unchanged when compared to existing conditions. Another possibility is that the Compatibility Plan may indirectly result in shifting future housing, facilities, and infrastructure to other locations designated or zoned to allow for such uses. Because such shifts, if they do occur, cannot be accurately predicted, particularly as to rate, timing, location, and extent, it is not considered reasonable to conclude that such shifts, if any, will be significant. Accordingly,

absent information to the contrary, any such shifts are reasonably considered less than significant.

In addition, nothing in the Compatibility Plan would result in indirect impacts like, for example, the construction of housing or other uses, or the extension of roads or other infrastructure, all of which would necessitate an analysis of potentially significant impacts to biological resources. Moreover, the Compatibility Plan would not result in the displacement of existing housing, structures, or people, necessitating the construction of replacement housing, facilities, or infrastructure, which may result in potentially significant impacts to biological resources. Therefore, any further analysis necessarily will occur if future shifts in development or construction result in a “project” subject to further CEQA compliance. All such projects will be required to comply with CEQA, necessitating preparation of a negative declaration, a mitigated negative declaration, or an EIR. This project-level environmental documentation ensures that if future shifts in development/construction indirectly result from the Compatibility Plan, these subsequent actions will not evade further project-level environmental review.

Further, as noted above, because implementation of the Compatibility Plan would not increase the levels of development in the AIA above those projected in the City of Hemet General Plan (including its constituent Specific Plans) and the Riverside County General Plan (specifically the Harvest Valley/Winchester Area Plan), and because the environmental effects of such development were already adequately analyzed at the General Plan level, there is no substantial evidence that adoption of the Compatibility Plan would result in any significant indirect impacts on biological resources.

(Sources: City General Plan and related Hemet EIR; County General Plan and related County EIR No. 441.)

PLAN MITIGATION: No mitigation necessary.

OTHER APPLICABLE/ADOPTED MITIGATION MEASURES: Development within unincorporated areas of Riverside County shall comply with Mitigation Measures 4.6.1A, 4.6.1B, 4.6.1C, 4.6.2A, 4.6.3A, 4.6.4A, 4.6.4B, 4.6.5A, 4.6.5B, 4.6.6A, 4.6.6B, 4.6.7A, 4.6.7B, 4.6.7C, 4.6.7D, and 4.6.7E, as specified in EIR No. 441.

CULTURAL RESOURCES

Issues	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
(a) Cause a substantial adverse change in the significance of a historical resource as defined in CEQA Guidelines Section 15064.5?			X	
(b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to CEQA Guidelines Section 15064.5?			X	
(c) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?			X	
(d) Disturb any human remains, including those interred outside of formal cemeteries?			X	

ANALYSIS:

Thresholds (a) - (d): The Compatibility Plan does not propose or entail any new development, construction, or physical changes to existing land uses or the environment. Therefore, the Compatibility Plan would not cause a substantial adverse change in the significance of a historical resource or an archaeological resource; directly or indirectly destroy a unique paleontological resource or site or unique geologic feature; or disturb any human remains, including those interred outside of formal cemeteries. As such, the project would not directly impact the environment or result in any direct impacts to cultural resources. Further, the Compatibility Plan would not increase the levels of development in the affected areas of the City of Hemet and unincorporated areas of the County of Riverside above those projected for these areas in the City of Hemet General Plan and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), the environmental effects of which were already adequately analyzed in the certified Hemet EIR and County EIR No. 441.

The Compatibility Plan may indirectly influence future land use development in the vicinity of Hemet-Ryan Airport by facilitating development at some locations and constraining development at other locations. However, any potential indirect effect that may arise is uncertain from a timing and location standpoint, and it is speculative to anticipate the specific characteristics of any development or the types of impacts to cultural resources that would be associated with it. One possibility is that land uses in much of the Airport's environs would remain unchanged when compared to existing conditions. Another possibility is that the Compatibility Plan may indirectly result in shifting future housing, facilities, and infrastructure to other locations designated or zoned to allow for such uses. Because such shifts, if they do

occur, cannot be accurately predicted, particularly as to rate, timing, location, and extent, it is not considered reasonable to conclude that such shifts, if any, will be significant. Accordingly, absent information to the contrary, any such shifts are reasonably considered less than significant.

In addition, nothing in the Compatibility Plan would result in indirect impacts like, for example, the construction of housing or other uses, or the extension of roads or other infrastructure, all of which would necessitate an analysis of potentially significant impacts to cultural resources. Moreover, the Compatibility Plan would not result in the displacement of existing housing, structures, or people, thereby necessitating the construction of replacement housing, facilities, or infrastructure, which may result in potentially significant impacts to cultural resources. Therefore, any further analysis necessarily will occur if future shifts in development or construction result in a “project” subject to further CEQA compliance. All such projects will be required to comply with CEQA, necessitating preparation of a negative declaration, a mitigated negative declaration, or an EIR. This project-level environmental documentation ensures that if future shifts in development/construction indirectly result from the Compatibility Plan, these subsequent actions will not evade further project-level environmental review.

Further, as noted above, because implementation of the Compatibility Plan would not increase the levels of development in the AIA above those projected in the City of Hemet General Plan (including its constituent Specific Plans) and the Riverside County General Plan (specifically the Harvest Valley/Winchester Area Plan), and because the environmental effects of such development were already adequately analyzed at the General Plan level, there is no substantial evidence that adoption of the Compatibility Plan would result in any significant indirect impacts on cultural resources.

(Sources: City General Plan and related Hemet EIR; County General Plan and related County EIR No. 441.)

PLAN MITIGATION: No mitigation necessary.

OTHER APPLICABLE/ADOPTED MITIGATION MEASURES: Development within unincorporated Riverside County shall comply with Mitigation Measures 4.7.1A, 4.7.1B, and 4.7.1C, as specified in EIR No. 441.

GEOLOGY AND SOILS

Issues	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
(a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:			X	
(i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault?				
(ii) Strong seismic ground shaking?				
(iii) Seismic-related ground failure, including liquefaction?				
(iv) Landslides?				
(b) Result in substantial soil erosion or the loss of topsoil?			X	
(c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse?			X	
(d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?			X	
(e) Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?			X	

ANALYSIS:

Thresholds (a) - (e): The Compatibility Plan does not propose or entail any new development, construction, or physical changes to existing land uses or the environment. The project will not change topography or ground surface relief features, will not create cut or fill slopes, and involves no grading. The project does not involve land disturbance and, therefore, will not result

in a change in deposition, siltation, or erosion, or in an increase in wind erosion or blowsand. Therefore, the Compatibility Plan would not expose people or structures to potential substantial adverse effects involving rupture of a known earthquake fault, strong seismic ground shaking, seismic-related ground failure, liquefaction, or landslides; result in substantial soil erosion or the loss of topsoil; be located on a geologic unit or soil that is unstable, potentially resulting in on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse; be located on expansive soil; or have soils incapable of adequately supporting the use of septic tanks; as such, the project would not directly impact the environment or result in any direct impacts to geology and soils. Further, the Compatibility Plan would not increase the levels of development in the affected areas of the City of Hemet and unincorporated areas of the County of Riverside above those projected for these areas in the City of Hemet General Plan and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), the environmental effects of which were already adequately analyzed in the certified Hemet EIR and County EIR No. 441.

The Compatibility Plan may indirectly influence future land use development in the vicinity of Hemet-Ryan Airport by facilitating development at some locations and constraining development at other locations. However, any potential indirect effect that may arise is uncertain from a timing and location standpoint, and it is speculative to anticipate the specific characteristics of any development or the types of impacts to geology and soils that would be associated with it. One possibility is that land uses in much of the Airport's environs would remain unchanged when compared to existing conditions. Another possibility is that the Compatibility Plan may indirectly result in shifting future housing, facilities, and infrastructure to other locations designated or zoned to allow for such uses. Because such shifts, if they do occur, cannot be accurately predicted, particularly as to rate, timing, location, and extent, it is not considered reasonable to conclude that such shifts, if any, will be significant. Accordingly, absent information to the contrary, any such shifts are reasonably considered less than significant.

In addition, nothing in the Compatibility Plan would result in indirect impacts like, for example, the construction of housing or other uses, or the extension of roads or other infrastructure, all of which would necessitate an analysis of potentially significant impacts to geology and soils. Moreover, the Compatibility Plan would not result in the displacement of existing housing, structures, or people, thereby necessitating the construction of replacement housing, facilities, or infrastructure, which may result in potentially significant impacts to geology and soils. Therefore, any further analysis necessarily will occur if future shifts in development or construction result in a "project" subject to further CEQA compliance. All such projects will be required to comply with CEQA, necessitating preparation of a negative declaration, a mitigated negative declaration, or an EIR. This project-level environmental documentation ensures that if future shifts in development/construction indirectly result from the Compatibility Plan, these subsequent actions will not evade further project-level environmental review.

Further, as noted above, because implementation of the Compatibility Plan would not increase the levels of development in the AIA above those projected in the City of Hemet General Plan (including its constituent Specific Plans) and the Riverside County General Plan (specifically the Harvest Valley/Winchester Area Plan), and because the environmental effects of such development were already adequately analyzed at the General Plan level, there is no substantial

evidence that adoption of the Compatibility Plan would result in any significant indirect impacts on geology and soils.

(Sources: City General Plan and related Hemet EIR; County General Plan and related County EIR No. 441.)

PLAN MITIGATION: No mitigation necessary.

OTHER APPLICABLE/ADOPTED MITIGATION MEASURES: Development within unincorporated areas of Riverside County shall comply with Mitigation Measures 4.10.2A, 4.10.2B, 4.10.2C, 4.10.3A, 4.10.3B, 4.10.7A, 4.10.8A, 4.10.9A, 4.10.9B, and 4.10.9C, as specified in EIR No. 441, and with applicable Land Use Element and Safety Element policies.

GREENHOUSE GAS EMISSIONS

Issues	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
(a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?			X	
(b) Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?			X	

ANALYSIS:

Thresholds (a) - (b): The Compatibility Plan does not propose or entail any new development, construction, or physical changes to existing land uses or the environment. Therefore, the Compatibility Plan would not generate greenhouse gas emissions or conflict with an applicable plan, policy, or regulation adopted to reduce greenhouse gas emissions; as such, the project would not directly impact the environment or result in any direct impacts to global climate change. Further, the Compatibility Plan would not increase the levels of development in the affected areas of the City of Hemet and unincorporated areas of the County of Riverside above those projected for this area in the City of Hemet General Plan and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), the environmental effects of which were already adequately analyzed in the certified Hemet EIR and County EIR No. 441.

The Compatibility Plan may indirectly influence future land use development in the vicinity of Hemet-Ryan Airport by facilitating development at some locations and constraining development at other locations. However, any potential indirect effect that may arise is uncertain from a timing and location standpoint, and it is speculative to anticipate the specific

characteristics of any development or the types of impacts to global climate change that would be associated with it. One possibility is that land uses in much of the Airport's environs would remain unchanged when compared to existing conditions. Another possibility is that the Compatibility Plan may indirectly result in shifting future housing, facilities, and infrastructure to other locations designated or zoned to allow for such uses. Because such shifts, if they do occur, cannot be accurately predicted, particularly as to rate, timing, location, and extent, it is not considered reasonable to conclude that such shifts, if any, will be significant. Accordingly, absent information to the contrary, any such shifts are reasonably considered less than significant.

In addition, nothing in the Compatibility Plan would result in indirect impacts like, for example, the construction of housing or other uses, or the extension of roads or other infrastructure, all of which would necessitate an analysis of potentially significant impacts to global climate change. Moreover, the Compatibility Plan would not result in the displacement of existing housing, structures, or people, thereby necessitating the construction of replacement housing, facilities, or infrastructure, which may result in potentially significant impacts to global climate change. Therefore, any further analysis necessarily will occur if future shifts in development or construction result in a "project" subject to further CEQA compliance. All such projects will be required to comply with CEQA, necessitating preparation of a negative declaration, a mitigated negative declaration, or an EIR. This project-level environmental documentation ensures that if future shifts in development/construction indirectly result from the Compatibility Plan, these subsequent actions will not evade further project-level environmental review.

Further, as noted above, because implementation of the Compatibility Plan would not increase the levels of development in the AIA above those projected in the City of Hemet General Plan (including its constituent Specific Plans) and the Riverside County General Plan (specifically the Harvest Valley/Winchester Area Plan), and because the environmental effects of such development were already adequately analyzed at the General Plan level, there is no substantial evidence that adoption of the Compatibility Plan would result in any significant indirect impacts relative to greenhouse gas emissions.

(Sources: City General Plan and related Hemet EIR; County General Plan and related County EIR No. 441.)

PLAN MITIGATION: No mitigation necessary.

OTHER APPLICABLE/ADOPTED MITIGATION MEASURES: The City of Hemet already has committed to mitigate development-related greenhouse gas emissions (impacts on global climate change) through compliance with Greenhouse Gas Emissions Mitigation Measures 4.7-1 and 4.7-2, as specified in the Hemet EIR.

HAZARDS AND HAZARDOUS MATERIALS

Issues	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
(a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?			X	
(b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?			X	
(c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?			X	
(d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?			X	
(e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of the public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?			X	
(f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?			X	
(g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?			X	
(h) Expose people or structures to a significant risk of loss, injury, or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?			X	

ANALYSIS:

Thresholds (a) - (h): Airports are industrial uses and have the potential to create safety hazards related to aircraft activity. The objectives set forth in the State Aeronautics Act (Pub. Util. Code §§21670-21679.5) call for the Commission to protect public health, safety, and welfare by ensuring the orderly expansion of airports and the adoption of land use measures that minimize the public's exposure to excessive noise and safety hazards within areas around public airports to the extent that these areas are not already devoted to incompatible uses. (Pub. Util. Code §21670, subd. (a)(2).) Pursuant to the State Aeronautics Act, the proposed Compatibility Plan establishes the criteria by which safety hazards relating to aircraft activity would be evaluated. These criteria are intended to reduce the risk of exposure to the hazards of an off-airport aircraft accident by limiting residential densities and concentrations of people in locations near Hemet-Ryan Airport. Relatedly, the proposed Compatibility Plan seeks to minimize the consequences of an off-airport aircraft accident by requiring a percentage of the land area in critical locations near the airport to remain open and reasonably suitable for a survivable emergency landing. The risks of aircraft accident occurrence also are reduced by policies limiting the height of structures, trees, and other objects that might penetrate airport airspace as defined by Part 77 of the Federal Aviation Regulations.

The proposed Compatibility Plan does not propose or entail any new development, construction, or physical changes to existing land uses or the environment. Moreover, the Compatibility Plan does not involve the transport, use, or disposal of hazardous materials; the emission or handling of hazardous or acutely hazardous materials, substances, or waste; or the location of a building, structure, or public facility on a hazardous materials site compiled by the State of California pursuant to Government Code section 65962.5.

Therefore, the Compatibility Plan would not affect the incidence of hazardous material safety hazards in the area; result in hazardous emissions within one-quarter mile of an existing or proposed school; affect any sites included on a list of hazardous materials sites; or affect emergency response plans or the incidence of wildland fires in the area. Further, the proposed Compatibility Plan would decrease airport-related safety hazards by limiting development within the AIA; therefore, if adopted, the Compatibility Plan would result in a beneficial impact by reducing the exposure of people to airport-related safety hazards, including aircraft accidents, consistent with the objectives of the State Aeronautics Act. In summary, the proposed Compatibility Plan would not directly impact the environment or result in any direct impacts relating to hazards and hazardous materials; and, the Compatibility Plan would not increase the levels of development in the affected areas of the City of Hemet and unincorporated areas of the County of Riverside above those projected for these areas in the City of Hemet General Plan and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), the environmental effects of which were already adequately analyzed in the certified Hemet EIR and County EIR No. 441.

The Compatibility Plan may indirectly influence future land use development in the vicinity of Hemet-Ryan Airport by facilitating development at some locations and constraining development at other locations. However, any potential indirect effect that may arise is uncertain from a timing and location standpoint, and it is speculative to anticipate the specific

characteristics of any development or the types of impacts to hazards or hazardous materials that would be associated with it. One possibility is that land uses in much of the Airport's environs would remain unchanged when compared to existing conditions. Another possibility is that the Compatibility Plan may indirectly result in shifting future housing, facilities, and infrastructure to other locations designated or zoned to allow for such uses. Because such shifts, if they do occur, cannot be accurately predicted, particularly as to rate, timing, location, and extent, it is not considered reasonable to conclude that such shifts, if any, will be significant. Accordingly, absent information to the contrary, any such shifts are reasonably considered less than significant.

In addition, nothing in the Compatibility Plan would result in indirect impacts like, for example, the construction of housing or other uses, or the extension of roads or other infrastructure, all of which would necessitate an analysis of potentially significant impacts to hazards or hazardous materials. Moreover, the Compatibility Plan would not result in the displacement of existing housing, structures, or people, thereby necessitating the construction of replacement housing, facilities, or infrastructure, which may result in potentially significant impacts to hazards or hazardous materials. Therefore, any further analysis necessarily will occur if future shifts in development or construction result in a "project" subject to further CEQA compliance. All such projects will be required to comply with CEQA, necessitating preparation of a negative declaration, a mitigated negative declaration, or an EIR. This project-level environmental documentation ensures that if future shifts in development/construction indirectly result from the Compatibility Plan, these subsequent actions will not evade further project-level environmental review.

Further, as noted above, because implementation of the Compatibility Plan would not increase the levels of development in the AIA above those projected in the City of Hemet General Plan (including its constituent Specific Plans) and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), and because the environmental effects of such development were already adequately analyzed at the General Plan level, there is no substantial evidence that adoption of the Compatibility Plan would result in any significant indirect impacts on hazards or hazardous materials.

(Sources: City General Plan and related Hemet EIR; County General Plan and related County EIR No. 441.)

PLAN MITIGATION: No mitigation necessary.

OTHER APPLICABLE/ADOPTED MITIGATION MEASURES: Pursuant to Policy PS-4.8d. of the Public Safety Element of the City of Hemet General Plan and Note 1 of Table 4 of the Harvest Valley/Winchester Area Plan (Riverside County General Plan), any use that would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation is prohibited in the Hemet AIA.

HYDROLOGY AND WATER QUALITY

Issues	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
(a) Violate any water quality standards or waste discharge requirements?			X	
(b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?			X	
(c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site?			X	
(d) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site?			X	
(e) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?			X	
(f) Otherwise substantially degrade water quality?			X	
(g) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?			X	
(h) Place within a 100-year flood hazard area structures which would impede or redirect flood flows?			X	

HYDROLOGY AND WATER QUALITY

Issues	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
(i) Expose people or structures to a significant risk of loss, injury, or death involving flooding, including flooding as a result of the failure of a levee or dam?			X	
(j) Inundation by seiche, tsunami, or mudflow?			X	

ANALYSIS:

Thresholds (a) - (j): The Compatibility Plan does not propose or entail any new development, construction, or physical changes to existing land uses or the environment. Therefore, the Compatibility Plan would not place housing or structures in a 100-year flood hazard area; violate any water quality standards; affect groundwater supplies; substantially alter drainage patterns; or expose people or structures to a significant risk involving flooding, seiche, tsunami or mudflow; as such, the project would not directly impact the environment or result in any direct impacts to hydrology and water quality. Further, the Compatibility Plan would not increase the levels of development in the affected areas of the City of Hemet and unincorporated areas of the County of Riverside above those projected for these areas in the City of Hemet General Plan and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), the environmental effects of which were already adequately analyzed in the certified Hemet EIR and County EIR No. 441.

The Compatibility Plan may indirectly influence future land use development in the vicinity of Hemet-Ryan Airport by facilitating development at some locations and constraining development at other locations. However, any potential indirect effect that may arise is uncertain from a timing and location standpoint, and it is speculative to anticipate the specific characteristics of any development or the types of impacts to hydrology and water quality that would be associated with it. One possibility is that land uses in much of the Airport's environs would remain unchanged when compared to existing conditions. Another possibility is that the Compatibility Plan may indirectly result in shifting future housing, facilities, and infrastructure to other locations designated or zoned to allow for such uses. Because such shifts, if they do occur, cannot be accurately predicted, particularly as to rate, timing, location, and extent, it is not considered reasonable to conclude that such shifts, if any, will be significant. Accordingly, absent information to the contrary, any such shifts are reasonably considered less than significant.

In addition, nothing in the Compatibility Plan would result in indirect impacts like, for example, the construction of housing or other uses, or the extension of roads or other infrastructure, all of which would necessitate an analysis of potentially significant impacts to hydrology and water

quality. Moreover, the Compatibility Plan would not result in the displacement of existing housing, structures, or people, thereby necessitating the construction of replacement housing, facilities, or infrastructure, which may result in potentially significant impacts to hydrology and water quality. Therefore, any further analysis necessarily will occur if future shifts in development or construction result in a “project” subject to further CEQA compliance. All such projects will be required to comply with CEQA, necessitating preparation of a negative declaration, a mitigated negative declaration, or an EIR. This project-level environmental documentation ensures that if future shifts in development/construction indirectly result from the Compatibility Plan, these subsequent actions will not evade further project-level environmental review.

Further, as noted above, because implementation of the Compatibility Plan would not increase the levels of development in the AIA above those projected in the City of Hemet General Plan (including its constituent Specific Plans) and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), and because the environmental effects of such development were already adequately analyzed at the General Plan level, there is no substantial evidence that adoption of the Compatibility Plan would result in any significant indirect impacts on hydrology and water quality.

(Sources: City General Plan and related Hemet EIR; County General Plan and related County EIR No. 441.)

PLAN MITIGATION: No mitigation necessary.

OTHER APPLICABLE/ADOPTED MITIGATION MEASURES: Development within unincorporated areas of Riverside County shall comply with Mitigation Measures 4.9.1A, 4.9.1B, 4.9.1C, 4.9.1D, 4.9.2A, 4.9.2B, 4.9.2C, 4.9.2D, 4.15.4A, 4.17.2A, 4.17.5A, 4.17.5B, 4.17.5C, 4.17.5D, and 4.17.5E, as specified in EIR No. 441.

LAND USE AND PLANNING

Issues	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
(a) Physically divide an established community?			X	
(b) Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?			X	
(c) Conflict with any applicable habitat conservation plan or natural community conservation plan?			X	

ANALYSIS:

Thresholds (a) and (c): The Compatibility Plan does not propose or entail any new development, construction, or physical changes to existing land uses or the environment. Therefore, the Compatibility Plan would not physically divide an established community or conflict with any applicable habitat conservation plan or natural community conservation plan; as such, the project would not directly impact the environment or result in any direct impacts to land use and planning, with respect to thresholds (a) and (c). Further, the Compatibility Plan would not increase the levels of development in the affected areas of the City of Hemet and unincorporated areas of the County of Riverside above those projected for these areas in the City of Hemet General Plan and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), the environmental effects of which were already adequately analyzed in the certified Hemet EIR and County EIR No. 441.

The Compatibility Plan may indirectly influence future land use development in the vicinity of Hemet-Ryan Airport by facilitating development at some locations and constraining development at other locations. However, any potential indirect effect that may arise is uncertain from a timing and location standpoint, and it is speculative to anticipate the specific characteristics of any development or the types of land use and planning impacts that would be associated with it. One possibility is that land uses in much of the Airport's environs would remain unchanged when compared to existing conditions. Another possibility is that the Compatibility Plan may indirectly result in shifting future housing, facilities, and infrastructure to other locations designated or zoned to allow for such uses. Because such shifts, if they do occur, cannot be accurately predicted, particularly as to rate, timing, location, and extent, it is not considered reasonable to conclude that such shifts, if any, will be significant. Accordingly,

absent information to the contrary, any such shifts are reasonably considered less than significant.

In addition, nothing in the Compatibility Plan would result in indirect impacts like, for example, the construction of housing or other uses, or the extension of roads or other infrastructure, all of which would necessitate an analysis of potentially significant land use and planning impacts. Moreover, the Compatibility Plan would not result in the displacement of existing housing, structures, or people, thereby necessitating the construction of replacement housing, facilities, or infrastructure, which may result in potentially significant land use and planning impacts. Therefore, any further analysis necessarily will occur if future shifts in development or construction result in a “project” subject to further CEQA compliance. All such projects will be required to comply with CEQA, necessitating preparation of a negative declaration, a mitigated negative declaration, or an EIR. This project-level environmental documentation ensures that if future shifts in development/construction indirectly result from the Compatibility Plan, these subsequent actions will not evade further project-level environmental review.

Further, as noted above, because implementation of the Compatibility Plan would not increase the levels of development in the AIA above those projected in the City of Hemet General Plan (including its constituent Specific Plans) and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), and because the environmental effects of such development were already adequately analyzed at the General Plan level, there is no substantial evidence that adoption of the Compatibility Plan would result in any significant indirect impacts on land use and planning.

Threshold (b): The Compatibility Plan does not directly or indirectly conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project adopted for the purpose of avoiding or mitigating an environmental effect, for the reasons discussed above. However, to the extent that the Compatibility Plan conflicts with other General Plan land use plans, policies, or regulations (*i.e.*, those not adopted for the purpose of avoiding or mitigating an environmental effect), it may indirectly influence future land use development in the vicinity of Hemet-Ryan Airport by constraining development at some locations; thus, the Compatibility Plan has the potential to shift future development to areas outside the AIA or to less restrictive zones within the AIA (e.g., Airport Compatibility Zone E). The Compatibility Plan, if adopted, also would constrain the allowable land use designations found in the City and County General Plans, unless the local land use jurisdiction elected to overrule all or portions of the Compatibility Plan pursuant to the State Aeronautics Act as discussed below.

To the extent that such adoption would result in conflicts between the Compatibility Plan and the General Plan of a local land use jurisdiction, such conflict(s) would necessitate either an amendment to the applicable General Plan or an overrule by the local jurisdiction’s governing body (here, either the Hemet City Council or the Riverside County Board of Supervisors). Specifically, the City of Hemet and/or the County of Riverside, as applicable, may, consistent with Government Code section 65302.3, alleviate any perceived land use conflict that exists between the Compatibility Plan and its General Plan, and any applicable Specific Plans, by taking steps necessary to amend such plans, as necessary, within 180 days of the adoption of the Compatibility Plan. Alternatively, if either jurisdiction does not concur with any provision of the

Compatibility Plan, it may take steps to overrule the Commission by adopting findings pursuant to section 21676 of the Public Utilities Code. Such actions are within the responsibility and jurisdiction of the City of Hemet and/or the County of Riverside, and not the Commission.

The following analysis addresses potential inconsistencies between the Compatibility Plan and the existing City of Hemet and Riverside County General Plans with respect to non-residential land uses, and quantifies the potential displacement of non-residential land uses.²

Based upon an evaluation of the existing General Plan land use designations, within the Airport's AIA, there are inconsistencies between existing General Plan land use designations and the allowable intensities provided under the Compatibility Plan. **Figure 3** depicts the existing General Plan land use designations for the areas lying within the AIA.

Specifically, the Compatibility Plan would conflict with existing land use designations by applying non-residential intensity limitations to properties designated for commercial and industrial development. In Compatibility Zone A, non-residential development would be prohibited. In Compatibility Zone B1, non-residential intensity would be limited to 40 persons per acre; the limit would be 100 persons per acre in Compatibility Zones B2 and C, and 200 persons per acre in Compatibility Zone D. These limitations serve as new constraints on development located within the AIA that otherwise would be allowed under the land use designations of the City of Hemet and County of Riverside General Plans (and applicable Specific Plans).

Potential Future Non-Residential Displacement Under The Compatibility Plan (With Additional Compatibility Policies 2.1 and 2.2) – City of Hemet:

The methodology described below was used to determine the potential non-residential displacement that may occur in the City of Hemet.

In order to estimate potential future commercial and industrial displacement, it is necessary to make certain assumptions as to how land would be developed if the proposed non-residential intensity limitations were not present, based on the development levels anticipated in the City of Hemet General Plan. Based on the anticipated floor-area ratios identified in the City's General Plan (the "Target Intensity" column of Table 2.1 on page 2-19 of the Land Use Element), staff calculated the maximum square footage that could be accommodated per acre in each existing non-residential land use designation. (The floor-area ratio is calculated by dividing the building area square footage by the net site area square footage.)

Within each of these designations, staff determined the "Affected Acreage" located within each Compatibility Zone by calculating the total area of land within each land use designation and Compatibility Zone, excluding existing rights-of-way/roadways. (Of note, staff did not exclude any existing development or parcels currently built-out; it was conservatively assumed that all land within the AIA was vacant. In this way, staff identified the highest anticipated potential

² For an analysis of the potential displacement of residential uses, please see the analysis under Population and Housing.

build-out amount. As a result, the amount of potential displacement is considered significantly overstated in that it does not adjust for existing development located within the AIA, which would not be impacted by implementation of the Compatibility Plan.)

Staff then multiplied the “Affected Acreage” by the “Square Feet/Acre” anticipated under the General Plan (as determined by the floor-area ratio) to determine the “Target Build-Out” permitted under the General Plan. For purposes of this analysis, the floor-area ratios identified in **Table 1** were used by staff, pursuant to Table 2.1 of the City of Hemet General Plan Land Use Element. Based on the floor-area ratios utilized, staff calculated the maximum square feet that could be accommodated per acre in each land use designation, as also shown in **Table 1**.

Table 1:
Anticipated Build-Out in the City of Hemet Pursuant to its General Plan

General Plan Land Use Designation	Floor-Area Ratio	Square Feet per Acre Under General Plan
Neighborhood Commercial	0.25	10,890
Community Commercial	0.30	13,068
Business Park	0.35	15,246
Industrial	0.40	17,424
Regional Commercial	0.40	17,424
Office Professional	0.50	21,780
Mixed Use	See Note 1	See Note 1

¹ Table 2-1 of the Land Use Element in the City’s General Plan does not identify a “Target Intensity” for the Mixed Use land use designation. However, the Mixed Use land use designation is limited to areas within Compatibility Zone D. If one were to assume a 0.80 floor-area ratio, there would be no displacement within this designation.

Staff then determined the “Maximum Build-Out” permitted under the Compatibility Plan with Additional Compatibility Policies 2.1 and 2.2 by calculating the maximum square footage allowed per acre within each Compatibility Zone and each land use designation (which is calculated by multiplying the “people per acre” allowable under each Compatibility Zone by the maximum square footage per person allowable for each land use designation) and multiplying the maximum square footage by the “Affected Acreage.”

The difference between the “Target Build-Out” permitted under the General Plan and the “Maximum Build-Out” permitted under the Compatibility Plan is the potential non-residential displacement that may occur with implementation of the Compatibility Plan with Additional Compatibility Policies 2.1 and 2.2 (“Potential Displaced Build-Out”).

Table 2 presents the potential non-residential displacement that may occur in the AIA under the Compatibility Plan with Additional Compatibility Policies 2.1 and 2.2.

**Table 2:
Potential Non-Residential Displacement Under the Compatibility Plan**

Jurisdiction	General Plan Land Use Designation	Airport Zone ¹	Affected Acreage	Target Build-Out Under General Plan		Maximum Build-Out Under Compatibility Plan		Potential Displaced Build-Out	
				Sq Ft / Acre	Target Build-Out (Sq Ft)	Sq Ft / Acre ²	Maximum Build-Out (Sq Ft)	(Sq Ft)	%
City of Hemet	Community Commercial								
		B1	29.91	13,068	390,864	4,600	137,586	253,278	65%
		C	40.33	13,068	527,032	11,500	463,795	63,237	12%
		D	338.49	13,068	4,423,387	23,000	4,423,387	0	0%
		Total	408.73	13,068	5,341,283	N/A	5,024,768	316,515	6%
	Neighborhood Commercial								
		C	0.95	10,890	10,346	11,500	10,346	0	0%
		D	1.02	10,890	11,108	23,000	11,108	0	0%
		Total	1.97	10,890	21,454	N/A	21,454	0	0%
	Office Professional								
		D/Total	14.68	21,780	319,730	40,000	319,730	0	0%
	Business Park								
		A	4.27	15,246	65,100	0	0	65,100	100%
		B1	47.56	15,246	725,100	8,000	380,480	344,620	48%
		B2	11.02	15,246	168,011	20,000	168,011	0	0%
		C	174.10	15,246	2,654,329	20,000	2,654,329	0	0%
		D	151.53	15,246	2,310,226	40,000	2,310,226	0	0%
		Total	388.48	15,246	5,922,766	N/A	5,513,046	409,720	7%
	Industrial								
		A	0.62	17,424	10,803	0	0	10,803	100%
		B1	49.06	17,424	854,821	8,000	392,480	462,341	54%
		B2	16.89	17,424	294,291	20,000	294,291	0	0%
		C	337.93	17,424	5,888,092	20,000	5,288,092	0	0%
		D	211.95	17,424	3,693,017	40,000	3,693,017	0	0%
		Total	616.45	17,424	10,741,024	N/A	10,267,880	473,145	5%
		TOTAL - City of Hemet			22,346,257		21,146,878	1,199,380	5%
	County of Riverside	Business Park							
		D	151.47	13,068	1,979,410	40,000	1,979,410	0	0%
TOTAL - County of Riverside					1,979,410		1,979,410	0	0%

**Table 2:
Potential Non-Residential Displacement Under the Compatibility Plan**

Jurisdiction	General Plan Land Use Designation	Airport Zone ¹	Affected Acreage	Target Build-Out Under General Plan		Maximum Build-Out Under Compatibility Plan		Potential Displaced Build-Out	
				Sq Ft / Acre	Target Build-Out (Sq Ft)	Sq Ft / Acre ²	Maximum Build-Out (Sq Ft)	(Sq Ft)	%

¹ Pursuant to Additional Compatibility Policy 2.1, Zone A does not permit any commercial/industrial uses, and is limited to 0 people per acre; Zone B1 is limited to 40 people per acre; Zones B2 and C are limited to 100 people per acre; and Zone D is limited to 200 people per acre.

² Maximum square footage per acre under the Compatibility Plan is calculated by multiplying "people per acre" allowable under each Compatibility Zone times the maximum square footage per person allowable for each General Plan land use designation. (E.g., Pursuant to Additional Compatibility Policy 2.1, Zone B1 is limited to 40 people per acre. Pursuant to Additional Compatibility Policy 2.2, mercantile uses are evaluated on the basis of one person per 115 square feet of gross building area (excluding restaurants, theaters, gymnasiums, and similar places of assembly, which have a higher level of intensity). Therefore, in Zone B1, using these assumptions, building size in such Commercial designations would need to be limited to 4,600 square feet in order to limit occupancy to 40 people.) Pursuant to Additional Compatibility Policy 2.2, one person would be accommodated for each 115 square feet of Community Commercial and Neighborhood Commercial uses.

Under the Compatibility Plan, one person would be accommodated for each 200 square feet of Professional Office uses. In this analysis, it is assumed that business park and industrial buildings would be able to accommodate one person per 200 square feet of floor area.

Summary Of Potential Future Non-Residential Displacement Under The Compatibility Plan With Additional Compatibility Policies 2.1 and 2.2:

As presented in **Table 2**, the total expected build-out for non-residential uses in the portion of the AIA in the City of Hemet, excluding Compatibility Zone E areas, is 22,346,257 square feet. The total potential future non-residential displacement that may result from implementation of the Compatibility Plan with Additional Compatibility Policies 2.1 and 2.2 is 1,199,380 square feet or 5.37 percent.

As noted above, these calculations are considered significantly overstated in that they have not accounted for the existing development or land uses that would otherwise qualify as existing uses under the Compatibility Plan. Nevertheless, a potential maximum future displacement of non-residential uses within the AIA of approximately 5.37 percent of the target build-out anticipated pursuant to the City General Plan is identified. Many of these potentially displaced uses could be accommodated within Airport Compatibility Zone E, or in surrounding areas.

In order to understand the scope and context of potential displacement, it is important to consider the potential displacement in a citywide context. The City of Hemet General Plan (Land Use Element, Table 2.3, page 2-27) indicates that 33,741,000 square feet of non-residential uses would be permitted at build-out. As noted above, the potential non-residential displacement that may result with implementation of the Compatibility Plan with the Additional Compatibility Policies is 1,199,380 square feet or 3.55 percent of the citywide potential build-out.

No non-residential displacement would occur in the unincorporated areas of Riverside County because the anticipated floor area ratio for the Business Park designation in the County area – 0.30, or 13,068 square feet of building area per acre - would accommodate less than 200 persons in an acre (the intensity limit within Compatibility Zone D), assuming office uses.

As discussed above, because the impacts of any such displacement are uncertain from a timing and location standpoint, it is speculative to anticipate the specific characteristics of any development or the types of land use and planning impacts that would be associated with it. Therefore, any potential physical environmental impacts occurring from possible displacement due to implementation of the Compatibility Plan are considered less than significant.

Moreover, although adoption of the Compatibility Plan would constrain development at some locations (and thereby inflate the potential demand at other locations), which may conflict with applicable General Plan land use designations, such designations were not adopted for the purpose of avoiding or mitigating an environmental effect, but rather to facilitate development of a wide range of non-residential uses. As a result, any potential physical environmental impacts associated with the Compatibility Plan's conflict with existing General Plan land use designations are considered less than significant, uncertain, and speculative.

Such conflicts also are considered less than significant under CEQA because state law (Gov. Code, §65302.3) requires that the General Plan, and any applicable Specific Plan, be consistent with an adopted Compatibility Plan, and, in the event of an inconsistency, the General Plan, and any applicable Specific Plan, must be promptly amended (or go through the special process required to overrule the Commission pursuant to section 21676 of the Public Utilities Code). The Commission finds that, even with adoption of the Compatibility Plan, any such land use conflicts can be avoided or substantially lessened by the City of Hemet taking prompt action to amend its General Plan land use designations, so that they are consistent with the adopted Compatibility Plan. The Commission further finds that such amendments are within the responsibility and jurisdiction of the City of Hemet and not the Commission.

Moreover, it is important to note that the Compatibility Plan is being adopted, pursuant to Public Utilities Code section 21670 *et seq.*, to protect public health, safety, and welfare, through the adoption of land use measures that minimize the public's exposure to excessive noise and safety hazards and is guided by the Handbook. As required by state law, the Compatibility Plan sets policies and criteria consistent with the State Aeronautics Act and within the parameters identified in the Handbook. Therefore, by its nature and pursuant to state law, adoption of the Compatibility Plan may necessitate restrictions on land uses within the AIA. These factors do not decrease the potential impact that the Compatibility Plan may have on future land uses and development, but they are nonetheless important considerations.

(Sources: City General Plan and related Hemet EIR; County General Plan and related County EIR No. 441.)

PLAN MITIGATION: No mitigation necessary.

OTHER APPLICABLE/ADOPTED MITIGATION MEASURES: None required.

MINERAL RESOURCES

Issues	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
(a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?			X	
(b) Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan, or other land use plan?			X	

ANALYSIS:

Thresholds (a) and (b): The Compatibility Plan does not propose or entail any new development, construction, or physical changes to existing land uses or the environment. Therefore, the Compatibility Plan would not result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state, or result in the loss of availability of a locally-important mineral resource recovery site. As such, the Compatibility Plan would not directly impact the environment or result in any direct impacts to mineral resources. Further, the Compatibility Plan would not increase the levels of development in the affected areas of the City of Hemet and unincorporated areas of the County of Riverside above those projected for this area in the City of Hemet General Plan and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), the environmental effects of which were already adequately analyzed in the certified Hemet EIR and County EIR No. 441.

The Compatibility Plan may indirectly influence future land use development in the vicinity of Hemet-Ryan Airport by facilitating development at some locations and constraining development at other locations. However, any potential indirect effect that may arise is uncertain from a timing and location standpoint, and it is speculative to anticipate the specific characteristics of any development or the types of impacts to mineral resources that would be associated with it. One possibility is that land uses in much of the Airport's environs would remain unchanged when compared to existing conditions. Another possibility is that the Compatibility Plan may indirectly result in shifting future housing, facilities, and infrastructure to other locations designated or zoned to allow for such uses. Because such shifts, if they do occur, cannot be accurately predicted, particularly as to rate, timing, location, and extent, it is not considered reasonable to conclude that such shifts, if any, will be significant. Accordingly, absent information to the contrary, any such shifts are reasonably considered less than significant.

In addition, nothing in the Compatibility Plan would result in indirect impacts like, for example, the construction of housing or other uses, or the extension of roads or other infrastructure, all of

which would necessitate an analysis of potentially significant impacts to mineral resources. Moreover, the Compatibility Plan would not result in the displacement of existing housing, structures, or people, thereby necessitating the construction of replacement housing, facilities, or infrastructure, which may result in potentially significant impacts to mineral resources. Therefore, any further analysis necessarily will occur if future shifts in development or construction result in a “project” subject to further CEQA compliance. All such projects will be required to comply with CEQA, necessitating preparation of a negative declaration, a mitigated negative declaration, or an EIR. This project-level environmental documentation ensures that if future shifts in development/construction indirectly result from the Compatibility Plan, these subsequent actions will not evade further project-level environmental review.

Further, as noted above, because implementation of the Compatibility Plan would not increase the levels of development in the AIA above those projected in the City of Hemet General Plan (including its constituent Specific Plans) and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), and because the environmental effects of such development were already adequately analyzed at the General Plan level, there is no substantial evidence that adoption of the Compatibility Plan would result in any significant indirect impacts on mineral resources.

(Sources: City General Plan and related Hemet EIR; County General Plan and related County EIR No. 441.)

PLAN MITIGATION: No mitigation necessary.

OTHER APPLICABLE/ADOPTED MITIGATION MEASURES: None required.

NOISE

Issues	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project result in:				
(a) Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?			X	
(b) Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?			X	
(c) A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?			X	

NOISE

Issues	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project result in:				
(d) A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?			X	
(e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?			X	
(f) For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?				X

ANALYSIS:

Thresholds (a) - (f): Airports are industrial uses and have the potential to create airport-related noise. Pursuant to the State Aeronautics Act, the Compatibility Plan establishes the criteria by which the public’s exposure to airport-related noise would be evaluated. These criteria are intended to reduce the public’s exposure to noise by limiting residential densities and concentrations of people in locations near Hemet-Ryan Airport. Pursuant to the Compatibility Plan, residential projects subject to review by the Commission and its staff within the 55 decibel (“dB”) Community Noise Equivalent Level (“CNEL”) contour at Hemet-Ryan Airport shall be subject to a condition requiring that noise attenuation measures be incorporated into residential construction to ensure that interior noise levels from aircraft operations are at or below 45 dB CNEL. (In most cases, mitigation to 45 dB CNEL may be achieved with standard construction if the exterior aircraft noise level is at or below 60 dB CNEL.) Residential projects subject to outdoor noise levels less than 55 dB CNEL are not considered to be impacted.

Notably, the Compatibility Plan does not propose or entail any new development, construction, or physical changes to existing land uses or the environment, and does not include any changes in aircraft or airport operations that would generate additional noise. Moreover, the Compatibility Plan would reduce exposure to airport-related noise by limiting development within the Hemet-Ryan Airport AIA. Therefore, the Compatibility Plan would not result in the exposure of people to increased noise or vibration levels; as such, the project would not directly impact the environment or result in any direct impacts related to noise. Indeed, the Compatibility Plan, if adopted, would result in a beneficial impact by reducing the exposure of people to increased noise levels, which is an important objective of the State Aeronautics Act.

Further, the Compatibility Plan would not increase the levels of development in the affected areas of the City of Hemet and unincorporated areas of the County of Riverside above those projected for these areas in the City of Hemet General Plan and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), the environmental effects of which were already adequately analyzed in the certified Hemet EIR and County EIR No. 441.

The Compatibility Plan may indirectly influence future land use development in the vicinity of Hemet-Ryan Airport by facilitating development at some locations and constraining development at other locations. However, any potential indirect effect that may arise is uncertain from a timing and location standpoint, and it is speculative to anticipate the specific characteristics of any development or the types of impacts related to noise that would be associated with it. One possibility is that land uses in much of the Airport's environs would remain unchanged when compared to existing conditions. Another possibility is that the Compatibility Plan may indirectly result in shifting future housing, facilities, and infrastructure to other locations designated or zoned to allow for such uses. Because such shifts, if they do occur, cannot be accurately predicted, particularly as to rate, timing, location, and extent, it is not considered reasonable to conclude that such shifts, if any, will be significant. Accordingly, absent information to the contrary, any such shifts are reasonably considered less than significant.

In addition, nothing in the Compatibility Plan would result in indirect impacts like, for example, the construction of housing or other uses, or the extension of roads or other infrastructure, all of which would necessitate an analysis of potentially significant impacts related to noise. Moreover, the Compatibility Plan would not result in the displacement of existing housing, structures, or people, necessitating the construction of replacement housing, facilities, or infrastructure, which may result in potentially significant impacts related to noise. Therefore, any further analysis necessarily will occur if future shifts in development or construction result in a "project" subject to further CEQA compliance. All such projects will be required to comply with CEQA, necessitating preparation of a negative declaration, a mitigated negative declaration, or an EIR. This project-level environmental documentation ensures that if future shifts in development/construction indirectly result from the Compatibility Plan, these subsequent actions will not evade further project-level environmental review.

Further, as noted above, because implementation of the Compatibility Plan would not increase the levels of development in the AIA above those projected in the City of Hemet General Plan (including its constituent Specific Plans) and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), and because the environmental effects of such development were already adequately analyzed at the General Plan level, there is no substantial evidence that adoption of the Compatibility Plan would result in any significant indirect impacts related to noise.

In summary, the Compatibility Plan may result in a reduction of the potential build-out population of the AIA in relation to the levels that may be expected pursuant to the City of Hemet General Plan. This reduction could occur in the event that the City of Hemet amends its General Plan in order to make the General Plan land use designations consistent with the Compatibility Plan. If this reduction occurs, it is intended to achieve the objectives set forth in the State Aeronautics Act (Pub. Util. Code, §§21670-21679.5), which call for the Commission to protect public health, safety, and welfare by ensuring the orderly expansion of airports and the adoption of land use measures that minimize the public's exposure to excessive noise and safety hazards within areas around public airports to the extent that these areas are not already devoted to incompatible uses. (Pub. Util. Code, §21670, subd. (a)(2).)

(Sources: City General Plan and related Hemet EIR; County General Plan and related County EIR No. 441.)

PLAN MITIGATION: No mitigation necessary.

OTHER APPLICABLE/ADOPTED MITIGATION MEASURES: Adoption of this Compatibility Plan would result in the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan (as adjusted for this area pursuant to Additional Compatibility Policies) becoming applicable to the Hemet-Ryan Airport Influence Area. Policy 4.1.6(a) of the Countywide Policies limit allowable interior noise levels from aircraft operations to 45 dB CNEL in new residential construction. Achievement of this noise level may require incorporation of noise attenuation measures into residential construction.

The City of Hemet has committed to mitigating the impacts of aircraft and airport noise through compliance with Noise Mitigation Measure 4.11-5, as specified in the Hemet EIR.

Development within unincorporated areas of Riverside County shall comply with Mitigation Measures 4.13.1A, 4.13.1B, 4.13.2A, 4.13.2B, 4.13.2C, 4.13.2D, 4.13.3A, 4.13.3B, 4.13.3C, 4.13.4A, and 4.13.4B, as specified in EIR No. 441.

POPULATION AND HOUSING

Issues	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
(a) Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?			X	
(b) Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?			X	
(c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?			X	

ANALYSIS:

Threshold (a): The Compatibility Plan would not directly or indirectly induce population growth. (See discussion of **Thresholds (b) and (c)** below.) However, to the extent that the Compatibility Plan may indirectly influence future land use development in the vicinity of Hemet-Ryan Airport by facilitating development at some locations and constraining development at other locations, the Compatibility Plan has the potential to result in shifting future development and, therefore, has the potential to impact population growth.

An analysis was conducted to determine the amount of developable residential acreage, and the number of dwelling units that would be precluded from development if the City of Hemet and the County of Riverside were to amend their General Plan and applicable Specific Plans to establish designations consistent with the Compatibility Plan. The analysis was performed with the assistance of data provided by the Geographic Information Systems section of the Riverside County Information Technology Department, the City of Hemet Planning Department, and Mead and Hunt, Inc.

Based upon an evaluation of the existing City of Hemet General Plan land use designations (including Specific Plan designations) and County of Riverside General Plan (Harvest Valley/Winchester Area Plan) land use designations within the AIA, there are some inconsistencies between existing land use designations and the allowable densities provided under the Compatibility Plan. Specifically, the Compatibility Plan would conflict with existing land use designations by applying residential density limitations to residential properties in the AIA. In Compatibility Zone A, residential development would be prohibited. Residential density would be limited to 0.05 dwelling units per acre in Compatibility Zone B1, 0.1 dwelling

units per acre in Compatibility Zone B2, and 0.2 dwelling units per acre in Compatibility Zone C. The residential density limit would be ≤ 0.4 (minimum parcel size 2.5 acres) or ≥ 4.5 dwelling units per acre in Compatibility Zone D. These limitations serve as new constraints on development located within the AIA that otherwise would be allowed under the land use designations of the City of Hemet General Plan (and its constituent Specific Plans) and the Riverside County General Plan.

Potential Future Residential Displacement Under The Compatibility Plan:

The methodology described below was used to determine the potential residential displacement that may occur in the City of Hemet and unincorporated areas in the vicinity. This analysis relates to future dwelling units that could be allowed pursuant to the General Plan or applicable Specific Plans; no existing dwelling units would be displaced.

Staff first determined the “Net Affected Acreage” -- the amount of acreage in each General Plan designation that is designated for a density inconsistent with the residential policies in the Compatibility Plan, excluding parcels that could not be further divided based on their existing land use designations’ (see **Table 3**).³

In order to qualify as a “divisible parcel,” a property designated Low Density Residential (2 to 5 dwelling units per acre) by the City of Hemet or Medium Density Residential (2 to 5 dwelling units per acre) by the County of Riverside would need to be at least 0.40 acre in area.

Residentially designated properties that are designated for densities exceeding five dwelling units per acre located in Compatibility Zone D are not the subject of displacement analysis, since densities of at least 4.5 dwelling units per acre would be permissible in that Compatibility Zone. However, properties so designated in Compatibility Zone C would be subject to displacement.

All properties designated High Density Residential were treated as “divisible” if they were located at least partially within Compatibility Zone C.

Densities exceeding one dwelling unit per five acres would be prohibited in Compatibility Zone C, rendering both the division of lots less than ten acres in area and the establishment of multiple units – except pursuant to second unit permit processes – on such lots inconsistent. In contrast, within Compatibility Zone D, the division of land to establish lots smaller than 0.2 acre in area and the establishment of multiple units to attain a density of at least five dwelling units per acre are both actions that are consistent with the Compatibility Plan.

The analysis for residentially designated parcels in Compatibility Zone C was conducted as follows:

³ The displacement analysis relates to properties in specific areas where conflicts or potential conflicts between Compatibility Plan (Airport Zone) designations and General Plan land use designations may occur. No attempt was made to analyze areas where conflicts do not occur (e.g., in Zone E, where there is no limit on residential densities).

The sum of the acreages of divisible parcels within each designation constitutes the “Net Affected Acreage” for that designation. This number was then multiplied by the maximum density presently allowable pursuant to the City’s or County’s General Plan (“Allowable Density”) to determine the total potential number of developable units⁴ allowed under the existing General Plan (“Developable Units Under Existing General Plan”).

Staff then calculated the potential developable units under the Compatibility Plan (“Developable Units Under Compatibility Plan”) by multiplying the “Net Affected Acreage” by the density permitted under the Compatibility Plan (“Allowable Density Under Compatibility Plan”). The difference between the “Developable Units Under Existing General Plan” and “Developable Units Under Compatibility Plan” is the amount of residential dwelling units that are potentially displaced (“Potentially Displaced Units”).

The analysis for residentially designated parcels in Compatibility Zone D was calculated as follows:

For parcels designated Low Density Residential by the City of Hemet or Medium Density Residential by the County of Riverside, the maximum density allowed pursuant to the City of Hemet General Plan or the County of Riverside General Plan (Harvest Valley/Winchester Area Plan) does not exceed five dwelling units per acre. Under a “worst-case” scenario, where the local jurisdiction will not allow for development near the top of the density range, the density permitted under the Compatibility Plan would be limited to 0.4 dwelling units per acre. However, ideally, the City or County would guide development toward densities in the sub-range of 4.5 to 5.0 dwelling units per acre. At this sub-range, the future residential development would meet the proposed Compatibility Plan criteria, and there would then be no residential displacement in Compatibility Zone D.

Medium High Density Residential and High Density Residential designations would be considered compatible within Compatibility Zone D, as projects with densities exceeding 4.5 dwelling units per acre would be consistent with the Compatibility Plan; conversely, projects with densities less than 4.5 dwelling units per acre would be inconsistent with the Compatibility Plan. For this analysis, we assume that these areas will be developed at densities of at least 4.5 dwelling units per acre and that the Compatibility Plan does not result in displacement from those areas.

The analysis for residentially designated parcels within the unincorporated area of Riverside County was conducted as follows:

For parcels designated Low Density Residential, where the maximum allowable density is two dwelling units per acre, the “Allowable Density” pursuant to the County’s General Plan was set at 2.0 dwelling units per acre and the “Allowable Density Under Compatibility Plan” was set at 0.2 dwelling units per acre for sites in Compatibility Zone C and 0.4 dwelling units per acre for sites in Compatibility Zone D.

⁴ The term “developable unit” does not include existing dwelling units or vacant lots within recorded urban/suburban subdivisions.

For parcels designated Rural Community: Estate Density Residential, where the maximum allowable density is one dwelling unit per two acres, the “Allowable Density” pursuant to the County’s General Plan was set at 0.5 dwelling units per acre and the “Allowable Density Under Compatibility Plan” was set at 0.2 dwelling units per acre for sites in Compatibility Zone C and 0.4 dwelling units per acre for sites in Compatibility Zone D.

**Table 3:
Potential Residential Displacement Under the Compatibility Plan**

Airport Zone	General Plan Designation	Net Affected Acreage	Existing General Plan		Compatibility Plan		Potentially Displaced Units
			Allowable Density (DU/Acre)	Developable Units	Allowable Density (DU/Acre)	Developable Units	
City of Hemet							
B2	Low Density Residential	22.01	5	110	0.1	2	108
C	High Density Residential	26.39	30	792	0.2	5	787
C	Low Density Residential	42.50	5	213	0.2	8	205
D	Low Density Residential	586.09	5	2,930	<0.4 or >4.5	234 or 2,930	0 - 2,696
City of Hemet Sub-Total		676.99	N/A	4,045	N/A	249-2,945	1,100 - 3,796
County of Riverside (unincorporated)							
C	Rural Community: Estate Density Residential	79.12	0.50	40	0.2	15	25
D	Community Development: Low Density Residential	49.95	2.0	100	0.4	19	81
D	Rural Community: Estate Density Residential	588.31	0.50	294	0.4	235	59
Unincorporated County of Riverside Sub-Total		717.38	N/A	434	N/A	269	165
TOTAL		1,394.37	N/A	4,479	N/A	518 - 3,214	1,265 - 3,961

As presented in Table 3, the maximum number of future dwelling units within Compatibility Zones B2, C, and D (excluding one residence per existing residential lot not able to be further divided and development in areas of Compatibility Zone D designated for five or more dwelling units per acre), as permitted by existing land use designations, is 4,479. Under the Compatibility Plan, under the “worst-case” scenario, 518 developable units would be permitted. However, if the areas of Compatibility Zone D are developed at a density of five dwelling units per acre, 3,214 dwelling units would be permitted. Therefore, the total number of “Potentially Displaced

Units” ranges from 1,265 to 3,961. Approximately 71.6 percent of the maximum potential residential displacement under the “worst-case” scenario occurs in Compatibility Zone D. However, over 80 percent of the potential residential displacement under the “best-case” scenario occurs in Compatibility Zone C.

Much of the potential residential displacement is “paper” displacement only, with land located within an approved tentative tract or existing apartment complexes, care facilities, mobile home parks, and recreational vehicle parks. For example, the 22.01-acre area with a City General Plan land use designation of Low Density Residential located in Compatibility Zone B2 is a portion of the Hemet West Mobile Home Park. If that community continues into the future with no additional spaces proposed in the Zone B2 area, the potential displacement in the City is reduced by 108 dwelling units. Additionally, 10.05 acres of the 26.39 acres with a City General Plan land use designation of High Density Residential located in Compatibility Zone C are included in the existing 358-space Casa Del Sol Recreational Vehicle Park. If that community continues into the future with no additional spaces proposed in the Zone C area, the potential displacement in the City is reduced by 299 dwelling units. These two areas account for 37 percent of the potential displacement from the B2 and C Zones in the City of Hemet.

In order to understand the scope of the potential displacement, it is important to consider the displacement in a citywide context. The City of Hemet General Plan provides for a total of 44,814 dwelling units at build-out, so the “worst-case” potential displacement within the City (3,796 dwelling units) would be 8.47 percent of the citywide total. However, if the City implements the higher-density approach guiding development toward the upper end of the Low Density Residential designation at 4.5 to 5.0 dwelling units per acre, displacement within the City portion of the AIA (1,100 dwelling units) could be as little as 2.45 percent of the citywide total.

As discussed above, because the impacts of any such displacement are uncertain from a timing and location standpoint, it is speculative to anticipate the specific characteristics of any development or the types of population and housing impacts that would be associated with it. Therefore, any potential physical environmental impacts occurring from possible population and housing displacement due to implementation of the Compatibility Plan are considered less than significant.

Moreover, it is important to note that the Compatibility Plan is being adopted, pursuant to Public Utilities Code section 21670 *et seq.*, to protect public health, safety, and welfare, through the adoption of land use measures that minimize the public’s exposure to excessive noise and safety hazards and is guided by the Handbook. As required by state law, the Compatibility Plan sets policies and criteria consistent with the State Aeronautics Act and within the parameters identified in the Handbook. Therefore, by its nature and pursuant to state law, adoption of the Compatibility Plan may necessitate restrictions on land uses within the AIA. These factors do not decrease the potential impact that the Compatibility Plan may have on future land uses and development, but they are nonetheless important considerations.

Thresholds (b) and (c): The Compatibility Plan does not propose or entail any new development, construction, or physical changes to existing land uses or the environment.

Therefore, the Compatibility Plan would not displace substantial numbers of existing housing or displace substantial numbers of people. As such, the Compatibility Plan would not directly impact the environment or result in any direct impacts to population and housing, with respect to thresholds (b) and (c). Further, the Compatibility Plan would not increase the levels of development in the affected areas of the City of Hemet and unincorporated areas of the County of Riverside above those projected for these areas in the City of Hemet General Plan and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), the environmental effects of which were already adequately analyzed in the certified Hemet EIR and County EIR No. 441.

The Compatibility Plan may indirectly influence future land use development in the vicinity of Hemet-Ryan Airport by facilitating development at some locations and constraining development at other locations. However, any potential indirect effect that may arise is uncertain from a timing and location standpoint, and it is speculative to anticipate the specific characteristics of any development or the types of impacts to population and housing that would be associated with it. One possibility is that land uses in much of the Airport's environs would remain unchanged when compared to existing conditions. Another possibility is that the Compatibility Plan may indirectly result in shifting future housing, facilities, and infrastructure to other locations designated or zoned to allow for such uses. Because such shifts, if they do occur, cannot be accurately predicted, particularly as to rate, timing, location, and extent, it is not considered reasonable to conclude that such shifts, if any, will be significant. Accordingly, absent information to the contrary, any such shifts are reasonably considered less than significant.

In addition, nothing in the Compatibility Plan would result in indirect impacts like, for example, the construction of housing or other uses, or the extension of roads or other infrastructure, all of which would necessitate an analysis of potentially significant impacts to population and housing. Moreover, the Compatibility Plan would not result in the displacement of existing housing, structures, or people, thereby necessitating the construction of replacement housing, facilities, or infrastructure, which may result in potentially significant impacts to population and housing. Therefore, any further analysis necessarily will occur if future shifts in development or construction result in a "project" subject to further CEQA compliance. All such projects will be required to comply with CEQA, necessitating preparation of a negative declaration, a mitigated negative declaration, or an EIR. This project-level environmental documentation ensures that if future shifts in development/construction indirectly result from the Compatibility Plan, these subsequent actions will not evade further project-level environmental review.

Further, as noted above, because implementation of the Compatibility Plan would not increase the levels of development in the AIA above those projected in the City of Hemet General Plan (including its constituent Specific Plans) and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), and because the environmental effects of such development were already adequately analyzed at the General Plan level, there is no substantial evidence that adoption of the Compatibility Plan would result in any significant indirect impacts on population and housing.

(Source: City General Plan and related Hemet EIR; County General Plan and related County

EIR No. 441.)

PLAN MITIGATION: No mitigation necessary.

OTHER APPLICABLE/ADOPTED MITIGATION MEASURES: None required.

PUBLIC SERVICES

Issues	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
(a) Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, or the need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times, or other performance objectives for any of the public services:				
(i) Fire protection?			X	
(ii) Police protection?			X	
(iii) Schools?			X	
(iv) Parks?			X	
(v) Other public facilities?			X	

ANALYSIS:

Threshold (a): The Compatibility Plan does not propose or entail any new development, construction, or physical changes to existing land uses or the environment. Therefore, the Compatibility Plan would not result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, and would not induce substantial population growth in an area and, therefore, would not result in the need for new or physically altered governmental facilities. As such, the Compatibility Plan would not directly impact the environment or result in any direct impacts related to public services. Further, the Compatibility Plan would not increase the levels of development in the affected areas of the City of Hemet and unincorporated areas of the County of Riverside above those projected for these areas in the City of Hemet General Plan and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), the environmental effects of which were already adequately analyzed in the certified Hemet EIR and County EIR No. 441.

The Compatibility Plan may indirectly influence future land use development in the vicinity of Hemet-Ryan Airport by facilitating development at some locations and constraining development at other locations. However, any potential indirect effect that may arise is uncertain

from a timing and location standpoint, and it is speculative to anticipate the specific characteristics of any development or the types of impacts to public services that would be associated with it. One possibility is that land uses in much of the Airport's environs would remain unchanged when compared to existing conditions. Another possibility is that the Compatibility Plan may indirectly result in shifting future housing, facilities, and infrastructure to other locations designated or zoned to allow for such uses. Because such shifts, if they do occur, cannot be accurately predicted, particularly as to rate, timing, location, and extent, it is not considered reasonable to conclude that such shifts, if any, will be significant. Accordingly, absent information to the contrary, any such shifts are reasonably considered less than significant.

In addition, nothing in the Compatibility Plan would result in indirect impacts like, for example, the construction of housing or other uses, or the extension of roads or other infrastructure, all of which would necessitate an analysis of potentially significant impacts to public services. Moreover, the Compatibility Plan would not result in the displacement of existing housing, structures, or people, thereby necessitating the construction of replacement housing, facilities, or infrastructure, which may result in potentially significant impacts to public services. Therefore, any further analysis necessarily will occur if future shifts in development or construction result in a "project" subject to further CEQA compliance. All such projects will be required to comply with CEQA, necessitating preparation of a negative declaration, a mitigated negative declaration, or an EIR. This project-level environmental documentation ensures that if future shifts in development/construction indirectly result from the Compatibility Plan, these subsequent actions will not evade further project-level environmental review.

Further, as noted above, because implementation of the Compatibility Plan would not increase the levels of development in the AIA above those projected in the City of Hemet General Plan (including its constituent Specific Plans) and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), and because the environmental effects of such development were already adequately analyzed at the General Plan level, there is no substantial evidence that adoption of the Compatibility Plan would result in any significant indirect impacts on public services.

(Sources: City General Plan and related Hemet EIR; County General Plan and related County EIR No. 441.)

PLAN MITIGATION: No mitigation necessary.

OTHER APPLICABLE/ADOPTED MITIGATION MEASURES: Development within unincorporated areas of Riverside County shall comply with Mitigation Measures 4.15.2A and 4.15.2D, as specified in EIR No. 441.

RECREATION

Issues	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
(a) Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?			X	
(b) Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?			X	

ANALYSIS:

Thresholds (a) and (b): The Compatibility Plan does not propose or entail any new development, construction, or physical changes to existing land uses or the environment. Therefore, the Compatibility Plan would not increase the use of existing neighborhood and regional parks or other recreational facilities and does not require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment; as such, the project would not directly impact the environment or result in any direct impacts to recreation. Further, the Compatibility Plan would not increase the levels of development in the affected areas of the City of Hemet and unincorporated areas of the County of Riverside above those projected for these areas in the City of Hemet General Plan and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), the environmental effects of which were already adequately analyzed in the certified Hemet EIR and County EIR No. 441.

The Compatibility Plan may indirectly influence future land use development in the vicinity of Hemet-Ryan Airport by facilitating development at some locations and constraining development at other locations. However, any potential indirect effect that may arise is uncertain from a timing and location standpoint, and it is speculative to anticipate the specific characteristics of any development or the types of impacts related to recreation that would be associated with it. One possibility is that land uses in much of the Airport's environs would remain unchanged when compared to existing conditions. Another possibility is that the Compatibility Plan may indirectly result in shifting future housing, facilities, and infrastructure to other locations designated or zoned to allow for such uses. Because such shifts, if they do occur, cannot be accurately predicted, particularly as to rate, timing, location, and extent, it is not considered reasonable to conclude that such shifts, if any, will be significant. Accordingly, absent information to the contrary, any such shifts are reasonably considered less than significant.

In addition, nothing in the Compatibility Plan would result in indirect impacts like, for example, the construction of housing or other uses, or the extension of roads or other infrastructure, all of which would necessitate an analysis of potentially significant impacts related to recreation. Moreover, the Compatibility Plan would not result in the displacement of existing housing, structures, or people, thereby necessitating the construction of replacement housing, facilities, or infrastructure, which may result in potentially significant impacts related to recreation. Therefore, any further analysis necessarily will occur if future shifts in development or construction result in a “project” subject to further CEQA compliance. All such projects will be required to comply with CEQA, necessitating preparation of a negative declaration, a mitigated negative declaration, or an EIR. This project-level environmental documentation ensures that if future shifts in development/construction indirectly result from the Compatibility Plan, these subsequent actions will not evade further project-level environmental review.

Further, as noted above, because implementation of the Compatibility Plan would not increase the levels of development in the AIA above those projected in the City of Hemet General Plan (including its constituent Specific Plans) and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), and because the environmental effects of such development were already adequately analyzed at the General Plan level, there is no substantial evidence that adoption of the Compatibility Plan would result in any significant indirect impacts on recreation.

(Sources: City General Plan and related Hemet EIR; County General Plan and related County EIR No. 441.)

PLAN MITIGATION: No mitigation necessary.

OTHER APPLICABLE/ADOPTED MITIGATION MEASURES: None required.

TRANSPORTATION/TRAFFIC

Issues	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
(a) Conflict with an applicable plan, ordinance or policy establishing a measure of effectiveness for the performance of the circulation system, taking into account all modes of transportation, including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit?			X	
(b) Conflict with an applicable congestion management program, including, but not			X	

TRANSPORTATION/TRAFFIC

Issues	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?				
(c) Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks, or otherwise alter waterborne, rail, or air traffic?			X	
(d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?			X	
(e) Result in inadequate emergency access or access to nearby uses?			X	
(f) Conflict with adopted policies, plans, or programs regarding public transit, bikeways or pedestrian facilities, or otherwise substantially decrease the performance or safety of such facilities?			X	

ANALYSIS:

Thresholds (a) - (f): The Compatibility Plan does not propose or entail any new development, construction, or physical changes to existing land uses or the environment. Therefore, the Compatibility Plan would not: conflict with an applicable plan, ordinance or policy establishing a measure of effectiveness for the performance of the circulation system; conflict with an applicable congestion management program; result in a change in air traffic patterns; substantially increase design hazards; result in inadequate emergency access or access to nearby uses; or conflict with adopted policies, plans or programs regarding public transit, bikeways or pedestrian facilities. As such, the Compatibility Plan would not directly impact the environment or result in any direct impacts related to traffic. Further, the Compatibility Plan would not increase the levels of development in the affected areas of the City of Hemet and unincorporated areas of the County of Riverside above those projected for these areas in the City of Hemet General Plan and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), the

environmental effects of which were already adequately analyzed in the certified Hemet EIR and County EIR No. 441.

The Compatibility Plan may indirectly influence future land use development in the vicinity of Hemet-Ryan Airport by facilitating development at some locations and constraining development at other locations. However, any potential indirect effect that may arise is uncertain from a timing and location standpoint, and it is speculative to anticipate the specific characteristics of any development or the types of traffic impacts that would be associated with it. One possibility is that land uses in much of the Airport's environs would remain unchanged when compared to existing conditions. Another possibility is that the Compatibility Plan may indirectly result in shifting future housing, facilities, and infrastructure to other locations designated or zoned to allow for such uses. Because such shifts, if they do occur, cannot be accurately predicted, particularly as to rate, timing, location, and extent, it is not considered reasonable to conclude that such shifts, if any, will be significant. Accordingly, absent information to the contrary, any such shifts are reasonably considered less than significant.

In addition, nothing in the Compatibility Plan would result in indirect impacts like, for example, the construction of housing or other uses, or the extension of roads or other infrastructure, all of which would necessitate an analysis of potentially significant traffic impacts. Moreover, the Compatibility Plan would not result in the displacement of existing housing, structures, or people, thereby necessitating the construction of replacement housing, facilities, or infrastructure, which may result in potentially significant traffic impacts. Therefore, any further analysis necessarily will occur if future shifts in development or construction result in a "project" subject to further CEQA compliance. All such projects will be required to comply with CEQA, necessitating preparation of a negative declaration, a mitigated negative declaration, or an EIR. This project-level environmental documentation ensures that if future shifts in development/construction indirectly result from the Compatibility Plan, these subsequent actions will not evade further project-level environmental review.

Further, as noted above, because implementation of the Compatibility Plan would not increase the levels of development in the AIA above those projected in the City of Hemet General Plan (including its constituent Specific Plans) and the Riverside County General Plan (including the Harvest Valley/Winchester Area Plan), and because the environmental effects of such development were already adequately analyzed at the General Plan level, there is no substantial evidence that adoption of the Compatibility Plan would result in any significant indirect impacts on traffic.

(Sources: City General Plan and related Hemet EIR; County General Plan and related County EIR No. 441.)

PLAN MITIGATION: No mitigation necessary.

OTHER APPLICABLE/ADOPTED MITIGATION MEASURES: Development within unincorporated areas of Riverside County shall comply with Mitigation Measures 4.16.1A and 4.16.1B, as specified in EIR No. 441.

UTILITIES AND SERVICE SYSTEMS

Issues	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
Would the project:				
(a) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?			X	
(b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?			X	
(c) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effect?			X	
(d) Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?			X	
(e) Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demands in addition to the provider's existing commitments?			X	
(f) Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?			X	
(g) Comply with federal, state, and local statutes and regulations related to solid waste?			X	

ANALYSIS:

Thresholds (a) - (g): The Compatibility Plan does not propose or entail any new development, construction, or physical changes to existing land uses or the environment. Therefore, the Compatibility Plan would not result in the construction of new wastewater or storm water facilities, and would not require additional water supplies, or wastewater or landfill capacity. As such, the Compatibility Plan would not directly impact the environment or result in any direct impacts to utilities and service systems. Further, the Compatibility Plan would not increase the levels of development in the affected areas of the City of Hemet and unincorporated areas of the County of Riverside above those projected for these areas in the City of Hemet General Plan and

the Riverside County General Plan (Harvest Valley/Winchester Area Plan), the environmental effects of which were already adequately analyzed in the certified Hemet EIR and County EIR No. 441.

The Compatibility Plan may indirectly influence future land use development in the vicinity of Hemet-Ryan Airport by facilitating development at some locations and constraining development at other locations. However, any potential indirect effect that may arise is uncertain from a timing and location standpoint, and it is speculative to anticipate the specific characteristics of any development or the types of impacts to utilities and service systems that would be associated with it. One possibility is that land uses in much of the Airport's environs would remain unchanged when compared to existing conditions. Another possibility is that the Compatibility Plan may indirectly result in shifting future housing, facilities, and infrastructure to other locations designated or zoned to allow for such uses. Because such shifts, if they do occur, cannot be accurately predicted, particularly as to rate, timing, location, and extent, it is not considered reasonable to conclude that such shifts, if any, will be significant. Accordingly, absent information to the contrary, any such shifts are reasonably considered less than significant.

In addition, nothing in the Compatibility Plan would result in indirect impacts like, for example, the construction of housing or other uses, or the extension of roads or other infrastructure, all of which would necessitate an analysis of potentially significant impacts to utilities and service systems. Moreover, the Compatibility Plan would not result in the displacement of existing housing, structures, or people, thereby necessitating the construction of replacement housing, facilities, or infrastructure, which may result in potentially significant impacts to utilities and service systems. Therefore, any further analysis necessarily will occur if future shifts in development or construction result in a "project" subject to further CEQA compliance. All such projects will be required to comply with CEQA, necessitating preparation of a negative declaration, a mitigated negative declaration, or an EIR. This project-level environmental documentation ensures that if future shifts in development/construction indirectly result from the Compatibility Plan, these subsequent actions will not evade further project-level environmental review.

Further, as noted above, because implementation of the Compatibility Plan would not increase the levels of development in the AIA above those projected in the City of Hemet General Plan (including its constituent Specific Plans) and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), and because the environmental effects of such development were already adequately analyzed at the General Plan level, there is no substantial evidence that adoption of the Compatibility Plan would result in any significant indirect impacts on utilities and service systems.

(Sources: City General Plan and related Hemet EIR; County General Plan and related County EIR No. 441.)

PLAN MITIGATION: No mitigation necessary.

OTHER APPLICABLE/ADOPTED MITIGATION MEASURES: As to water supplies and treatment facilities, development within unincorporated areas of Riverside County shall comply with Mitigation Measures 4.17.1A, 4.17.1B, 4.17.1C, 4.17.1D, 4.17.2A, 4.17.3A, 4.17.4A, 4.17.4B, 4.17.4C, 4.17.5A, 4.17.5B, 4.17.5C, 4.17.5D, and 4.17.5E, as specified in EIR No. 441.

As to wastewater treatment facilities and sewer-related issues, development within unincorporated areas of Riverside County shall comply with Mitigation Measures 4.15.4A, 4.17.5A, and 4.17.5E, as specified in EIR No. 441.

As to solid wastes, development within unincorporated areas of Riverside County shall comply with Mitigation Measures 4.15.3E and 4.15.3F, as specified in EIR No. 441.

As to utilities generally, development within unincorporated areas of Riverside County shall comply with Mitigation Measures 4.8.1A and 4.8.1B, as specified in EIR No. 441.

MANDATORY FINDINGS OF SIGNIFICANCE

Issues	Potentially Significant Impact	Less Than Significant With Mitigation Incorporated	Less Than Significant Impact	No Impact
(a) Does the project have the potential to substantially degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal, or eliminate important examples of the major periods of California history or prehistory?			X	
(b) Does the project have impacts that are individually limited, but cumulatively considerable?			X	
(c) Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?			X	

ANALYSIS:

Thresholds (a) - (c): The Compatibility Plan does not propose or entail any new development, construction, or physical changes to existing land uses or the environment. Therefore, the

Compatibility Plan does not have the potential to substantially degrade the quality of the environment; substantially reduce the habitat of a fish or wildlife species; cause a fish or wildlife population to drop below self-sustaining levels; threaten to eliminate a plant or animal community; reduce the number or restrict the range of a rare or endangered plant or animal; eliminate important examples of the major periods of California history or prehistory; have impacts that are individually limited, but cumulatively considerable; or have environmental effects which will cause substantial adverse effects on human beings. As such, the Compatibility Plan would not directly impact the environment or result in any direct impacts in this regard. Further, the Compatibility Plan would not increase the levels of development in the affected areas of the City of Hemet and unincorporated areas of the County of Riverside above those projected for this area in the City of Hemet General Plan and the Riverside County General Plan (Harvest Valley/Winchester Area Plan), the environmental effects of which were already adequately analyzed in the certified Hemet EIR and County EIR No. 441.

The Compatibility Plan may indirectly influence future land use development in the vicinity of Hemet-Ryan Airport by facilitating development at some locations and constraining development at other locations. However, any potential indirect effect that may arise is uncertain from a timing and location standpoint, and it is speculative to anticipate the specific characteristics of any development or the types of impacts to wildlife, their habitats, important examples of California history, or human beings that would be associated with it. One possibility is that land uses in much of the Airport's environs would remain unchanged when compared to existing conditions. Another possibility is that the Compatibility Plan may indirectly result in shifting future housing, facilities, and infrastructure to other locations designated or zoned to allow for such uses. Because such shifts, if they do occur, cannot be accurately predicted, particularly as to rate, timing, location, and extent, it is not considered reasonable to conclude that such shifts, if any, will be significant. Accordingly, absent information to the contrary, any such shifts are reasonably considered less than significant.

In addition, nothing in the Compatibility Plan would result in indirect impacts like, for example, the construction of housing or other uses, or the extension of roads or other infrastructure, all of which would necessitate an analysis of potentially significant impacts to wildlife, their habitats, important examples of California history, or human beings. Moreover, the Compatibility Plan would not result in the displacement of existing housing, structures, or people, thereby necessitating the construction of replacement housing, facilities, or infrastructure, which may result in potentially significant impacts to wildlife, their habitats, important examples of California history, or human beings. Therefore, any further analysis necessarily will occur if future shifts in development or construction result in a "project" subject to further CEQA compliance. All such projects will be required to comply with CEQA, necessitating preparation of a negative declaration, a mitigated negative declaration, or an EIR. This project-level environmental documentation ensures that if future shifts in development/construction indirectly result from the Compatibility Plan, these subsequent actions will not evade further project-level environmental review.

Further, as noted above, because implementation of the Compatibility Plan would not increase the levels of development in the AIA above those projected in the City of Hemet General Plan (including its constituent Specific Plans) and the Riverside County General Plan (Harvest

Valley/Winchester Area Plan), and because the environmental effects of such development were already adequately analyzed at the General Plan level, there is no substantial evidence that adoption of the Compatibility Plan would result in any significant indirect impacts on wildlife, their habitats, important examples of California history, or human beings.

Lastly, because the Compatibility Plan is regulatory in nature and will not result in any new development, construction, or physical changes to existing land uses or the environment, it has no potential to create cumulatively significant environmental impacts. Indeed, the Compatibility Plan serves as a plan designed to avoid certain noise and safety impacts that might otherwise be cumulatively significant.

PLAN MITIGATION: No mitigation necessary.

OTHER APPLICABLE/ADOPTED MITIGATION MEASURES: None required.

EARLIER ANALYSES

Earlier analyses may be used where, pursuant to tiering, program EIR, or other CEQA process, one or more effects have been adequately analyzed in an earlier EIR or negative declaration. (Cal. Code Regs., tit.14, §15063, subd. (c)(3)(D).)

SOURCES

The following documents referenced in this Initial Study are incorporated by this reference and are available for public inspection and review, upon request to John J. G. Guerin, at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California, 92501:

CALIFORNIA DEPARTMENT OF TRANSPORTATION, DIVISION OF AERONAUTICS. California Airport Land Use Planning Handbook, 2011. (Also available for review at www.dot.ca.gov/hq/planning/aeronaut/htmlfile/landuse.php.)

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION. Riverside County Airport Land Use Compatibility Plan. October 14, 2004. (Also available for review at www.rcaluc.org/plan_new.asp.)

COUNTY OF RIVERSIDE, CALIFORNIA.

Riverside County General Plan (including the Harvest Valley/Winchester Area Plan). October 2003. (Also available for review at www.rctlma.org/generalplan/index.html.)

Riverside County General Plan Final Program Environmental Impact Report (Environmental Impact Report No. 441, State Clearinghouse No. 2002051143). October 2003. (Also available for review at www.rctlma.org/generalplan/index.html.)

The following documents referenced in this study are incorporated by this reference and are available for public inspection and review online at the City of Hemet website, or at the City of Hemet Planning Department, located at 445 E. Florida Avenue, Hemet, California 92543. (City of Hemet Planning Department is closed on Fridays.)

CITY OF HEMET, CALIFORNIA..

City of Hemet 2030 General Plan (Adopted January 24, 2012). (Also available for review at <http://www.cityofhemet.org/DocumentCenter/Home/View>.)

Final Environmental Impact Report for City of Hemet 2030 General Plan (State Clearinghouse No. 2010061088), January 12, 2012. (Also available for review at <http://www.cityofhemet.org/DocumentCenter/Home/View>.)

S 2016111015

Notice of Completion & Environmental Document Transmittal

Mail to: State Clearinghouse, P.O. Box 3044, Sacramento, CA 95812-3044 (916) 445-0613
 For Hand Delivery/Street Address: 1400 Tenth Street, Sacramento, CA 95814

Project Title: Hemet-Ryan Airport Land Use Compatibility Plan (Hemet ALUCP)

Lead Agency: Riverside County Airport Land Use Commission (RCALUC) Contact Person: John J. G. Guerin
 Mailing Address: 4080 Lemon Street, 14th Floor Phone: (951) 955-0982
 City: Riverside Zip: 92501 County: Riverside

Project Location: County Riverside City/Nearest Community: Hemet

Cross Streets: W. Stetson Ave and Walden Weaver Rd. Zip Code: _____

Longitude/Latitude (degrees, minutes and seconds): 33 °44 '2.4 " N / 117 °01 '21.2 " W Total Acres 440

Assessor's Parcel No.: 456-020-002 Section: 13 Twp.: 5 Range: 1 Base: _____

Within 2 Miles: State Hwy #: 74 & 79 Waterways: Diamond Valley Lake

Airports: Hemet-Ryan Railways: AT&SF Schools: Hemet USD

Document Type:

CEQA: NOP Draft EIR EIR Other: Joint Document
 Early Cons Supplement/Subsequent EIR EA Final Document
 Neg Dec (Prior SCH No.) _____ Draft LIS Other: _____
 Mit Neg Dec Other: _____

Governor's Office of Planning & Research
 NOV 04 2016

STATE CLEARINGHOUSE

Local Action Type:

General Plan Update Specific Plan Rezone Annexation
 General Plan Amendment Master Plan Prezone Redevelopment
 General Plan Element Planned Unit Development Use Permit Coastal Permit
 Community Plan Site Plan Land Division (Subdivision, etc.) Other: ALUCP

Development Type:

Residential: Units _____ Acres _____
 Office: Sq.ft. _____ Acres _____ Employees _____
 Commercial: Sq.ft. _____ Acres _____ Employees _____
 Industrial: Sq.ft. _____ Acres _____ Employees _____
 Educational: _____
 Recreational: _____
 Water Facilities: Type _____ MGD _____

Transportation: Type _____
 Mining: Mineral _____
 Power: Type _____ MW _____
 Waste Treatment: Type _____ MGD _____
 Hazardous Waste: Type _____
 Other: ALUCP

Project Issues Discussed in Document:

Aesthetic/Visual Fiscal Recreation/Parks Vegetation
 Agricultural Land Flood Plain/Flooding Schools/Universities Water Quality
 Air Quality Forest Land/Fire Hazard Septic Systems Water Supply/Groundwater
 Archeological/Historical Geologic/Seismic Sewer Capacity Wetland/Riparian
 Biological Resources Minerals Soil Erosion/Compaction/Grading Growth Inducement
 Coastal Zone Noise Solid Waste Land Use
 Drainage/Absorption Population/Housing Balance Toxic/Hazardous Cumulative Effects
 Economic/Jobs Public Services/Facilities Traffic/Circulation Other: _____

Present Land Use/Zoning/General Plan Designation:

Various within City of Hemet and unincorporated areas of Riverside County including commercial, office, industrial, residential

Project Description: (please use a separate page if necessary)

The proposed project is the RCALUC's adoption of an ALUCP that would be applicable to the environs of Hemet-Ryan Airport. The proposed ALUCP includes "Additional Compatibility Policies" tailored specifically to the Airport's land use environs and reflects the Commission's efforts to develop an ALUCP that fully satisfies the objectives of the State Aeronautics Act and addresses local concerns. The ALUCP establishes policies for determining the consistency of future, proposed development projects within the Hemet-Ryan Airport Influence Area with the objective of protecting public health, safety, and welfare, as set forth in the State Aeronautics Act (Pub. Util. Code, §521670-21679.5).

Note: The State Clearinghouse will assign identification numbers for all new projects. If a SCH number already exists for a project (e.g. Notice of Preparation or previous draft document) please fill in.

CITY OF HEMET PLANNING STAFF PRESENTATION TO HEMET CITY COUNCIL

Hemet-Ryan Airport Land Use Plan (ALUP)

Hemet City Council Work Study

November 15, 2016

Hemet-Ryan Airport



Legislative Background

- State law mandates all counties to have a functioning Airport Land Use Commission (ALUC), and to establish Airport Land Use Plans (ALUP) for every airport within their jurisdiction.
- Caltrans Division of Aeronautics administers the statute and the Airport Land Use Planning “Handbook” to provide guidance for the formation of local Airport Land Use Plans.
- The County of Riverside adopted a Countywide Airport Land Use Plan in 2004, which serves as a baseline policy document for all the local plans. Each individual ALUP can include modified or supplemental policies for a particular airport.
- The existing Hemet-Ryan Airport Comprehensive Airport Land Use Plan was adopted in 1992, and pre-dates the Countywide Policies.
- Riverside County ALUC is proposing a new 2016 Hemet Ryan ALUP which is set for public hearing before ALUC on Dec. 8, 2016.

Work Study Purpose

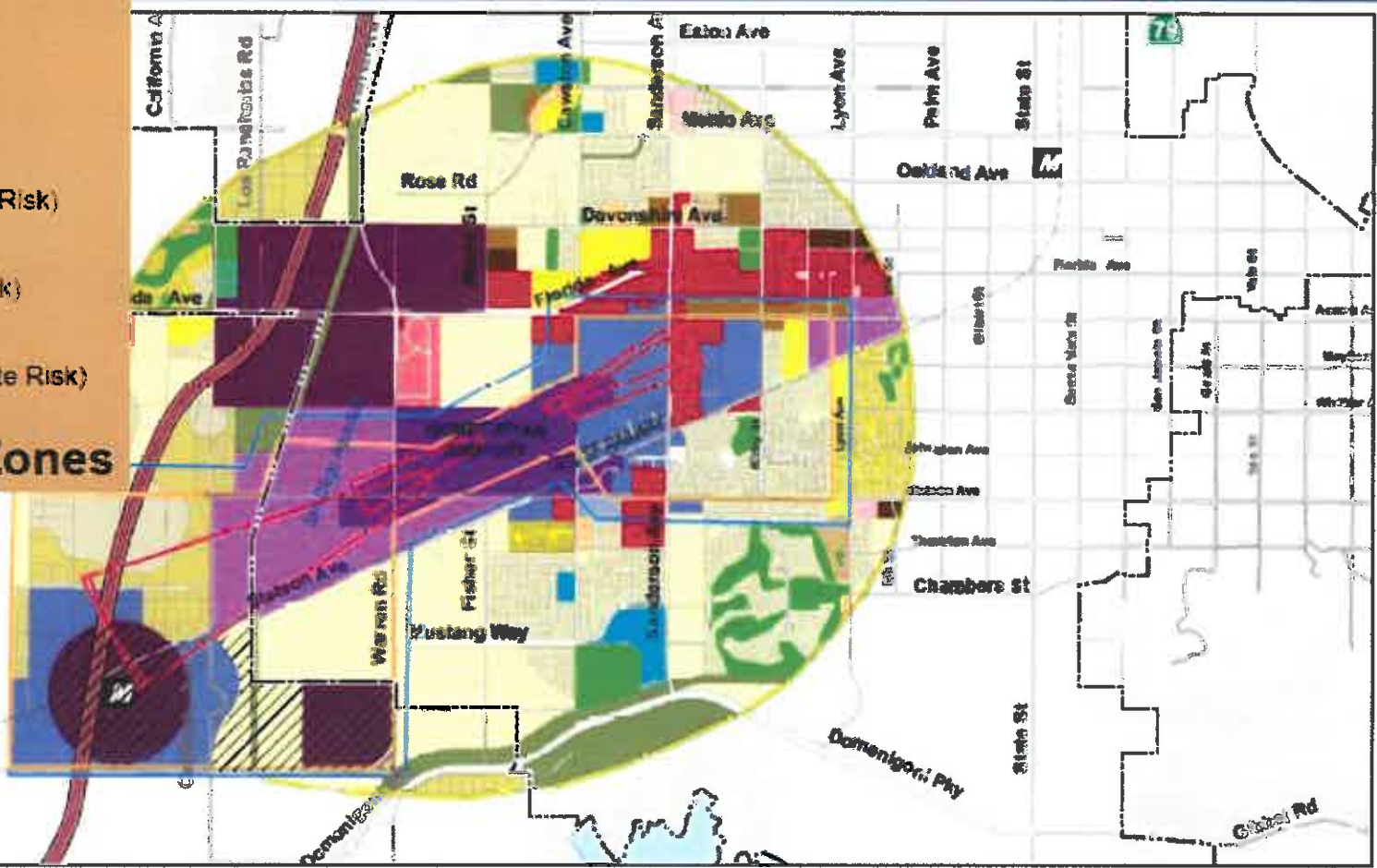
- Introduce the Council to the proposed Plan
- Provide an overview of the changes between the existing 1992 Plan and the proposed 2016 Plan
- Present Staff's preliminary review and concerns for Council direction regarding formal comments to the ALUC on the Plan:
 - *Public Review period for the proposed ALUP and Negative Declaration : November 7th – December 7th*
 - *ALUC Public Hearing on the Plan: December 8th*



1992 Hemet-Ryan Airport Plan Zones

Compatibility Zones

-  City Boundary
-  Transition Area
-  Area I (Extreme Risk)
-  Area II (High Risk)
-  Area III (Moderate Risk)



Notes: Zones subject to change based on updates to Federal - Ryan Airport Master Plan and Riverside County Airport Land Use Plan.

Source: Oregon Tiger Plan Data 2003
 Bureau of Aeronautics, Airport Compatibility Studies
 Airport Land Use Plan, 1992
 ES&S 2009



LEGEND








 Area I - Extreme Risk	 General: High Density Residential	 Very High Density Residential	 Public Use	 General City East Side
 Area II - High Risk	 General: Medium Density Residential	 Regional Commercial	 Airport	 Stream
 Transition Area	 General: Low Density Residential	 Neighborhood Commercial	 Public Facilities	 Railroad
 Area III - Moderate Risk	 General: Low Medium Density Residential	 Community Commercial	 School	 Creek/Canal
 General Airport Overlay Zone	 General: Medium Density Residential	 Mixed Use	 Park	 Major Arterial
	 General: High Density Residential	 Office/Professional & General	 Open Space	 Major Freeway

Figure 2.88
AIRPORT LAND USE
COMPATIBILITY ZONES
 Hemet General Plan

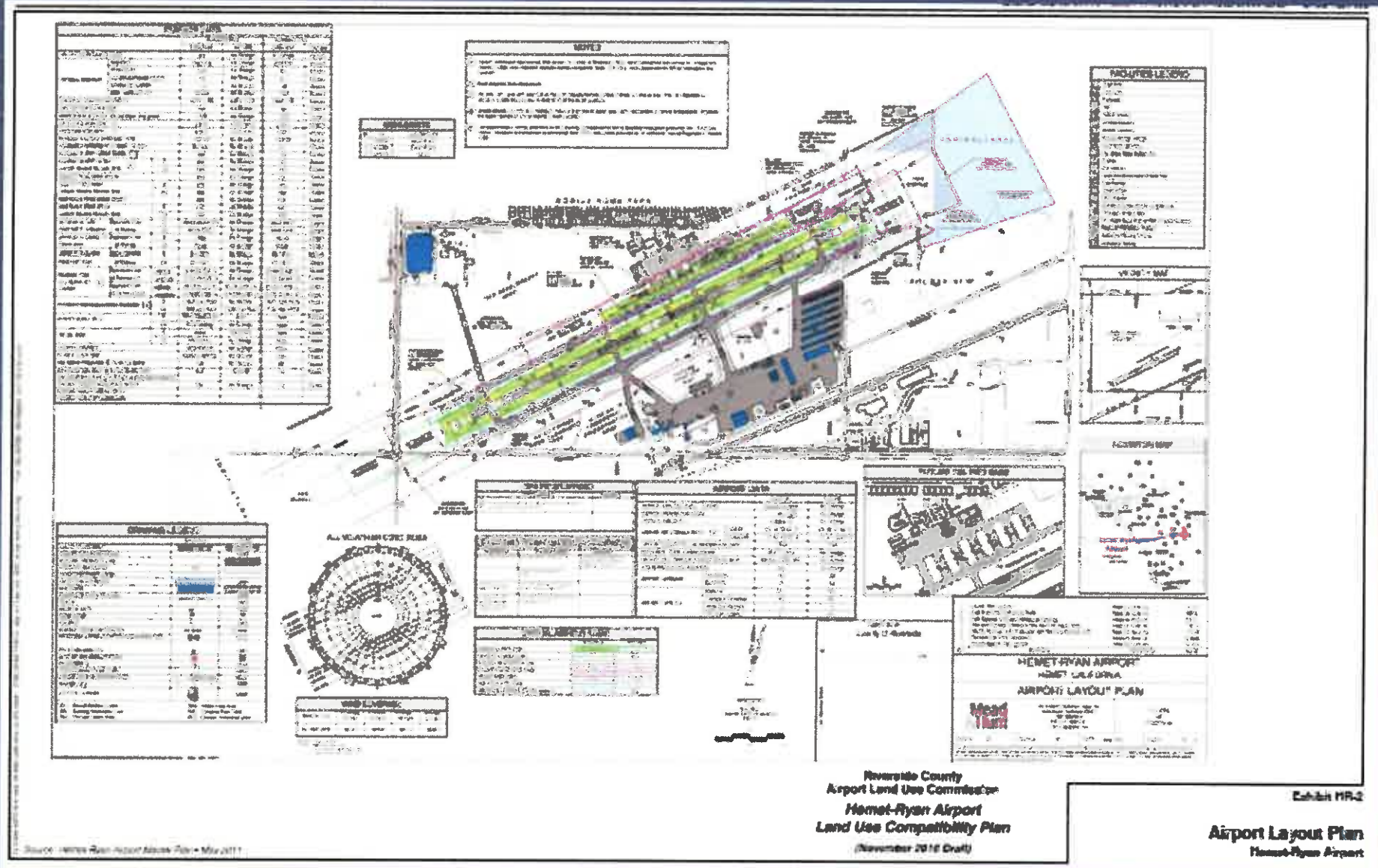
1992 Hemet-Ryan Airport Plan Zones

1992 Compatibility Zones

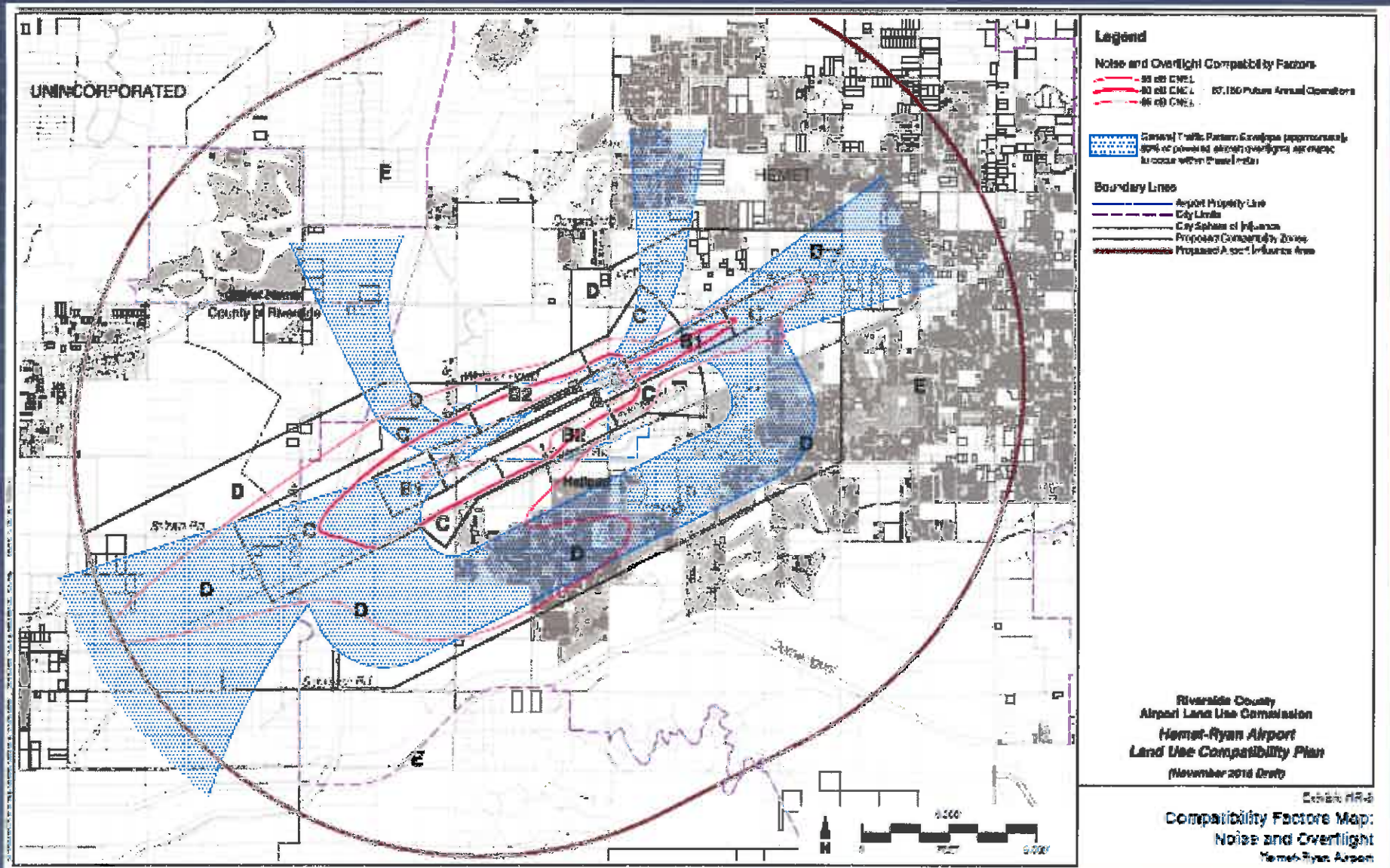
Zone	Definition	Land Uses
Area I	Extreme Risk -High volume of aircraft	Agriculture & Open Space
Area II	High Risk - High volume with turning movements	Residential density at 1 du/2.5 ac., Some restrictions of commercial/industrial intensity.
Transition Area	330 feet inside Area II boundary and 660 feet outside of Area II.	Residential density not more than 20 d.u./ac. Commercial, industrial uses permitted. Some restrictions on places of assembly.
Area III	Moderate Risk	No land use restriction. Disclosure requirements of airport location.

Airport Master Plan

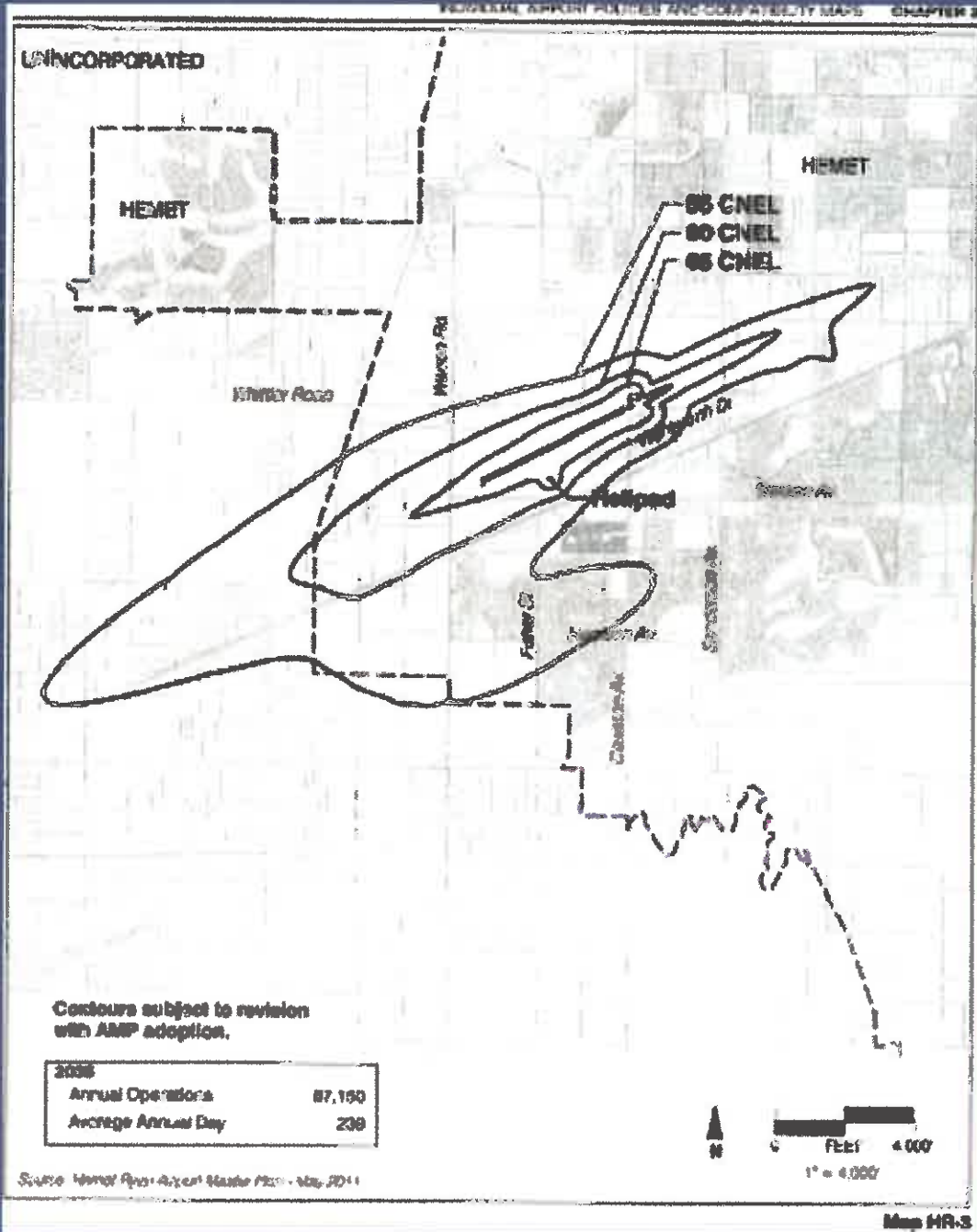
- No changes in airport operations proposed at this time. Primarily small recreational aircraft, helicopters, and Cal Fire aircraft
- No planned expansion of the runway to the West due to MSHCP and habitat concerns
- Proposed “Displaced Threshold” of 500 foot runway extension to the East, to assist in take off
- Primary changes are related to application of the Countywide Airport Policies and establishment of 6 Airport Zones, similar to the Caltrans model



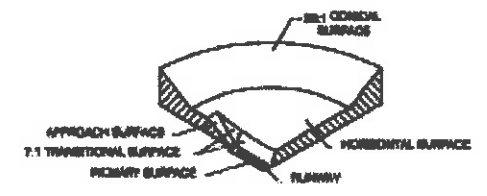
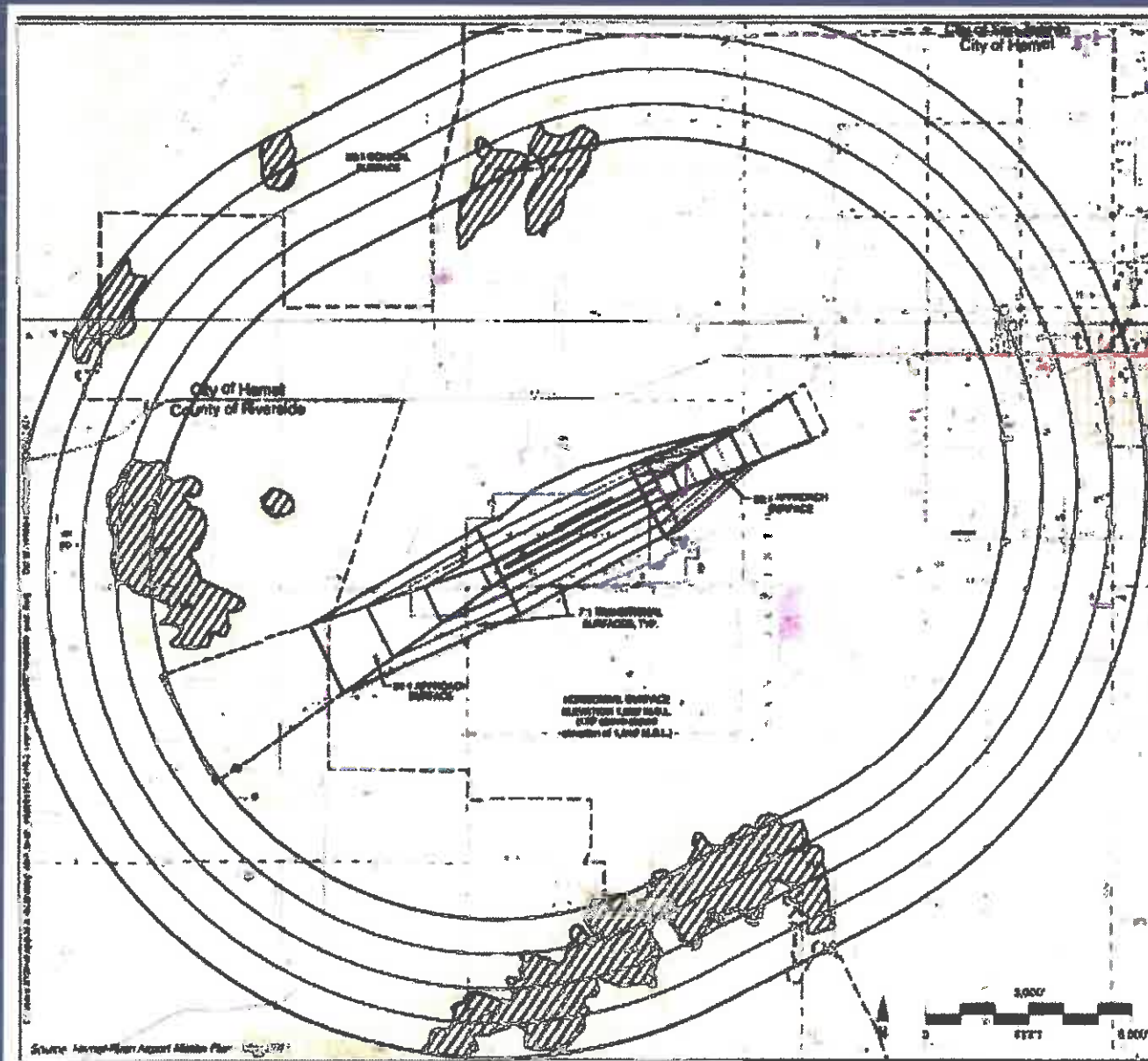
Hemet-Ryan Airport Master Plan



Compatibility Factors (Noise & Overflight)



Projected Noise Contours - 2036



TYPICAL FAR PART 77 SURFACES

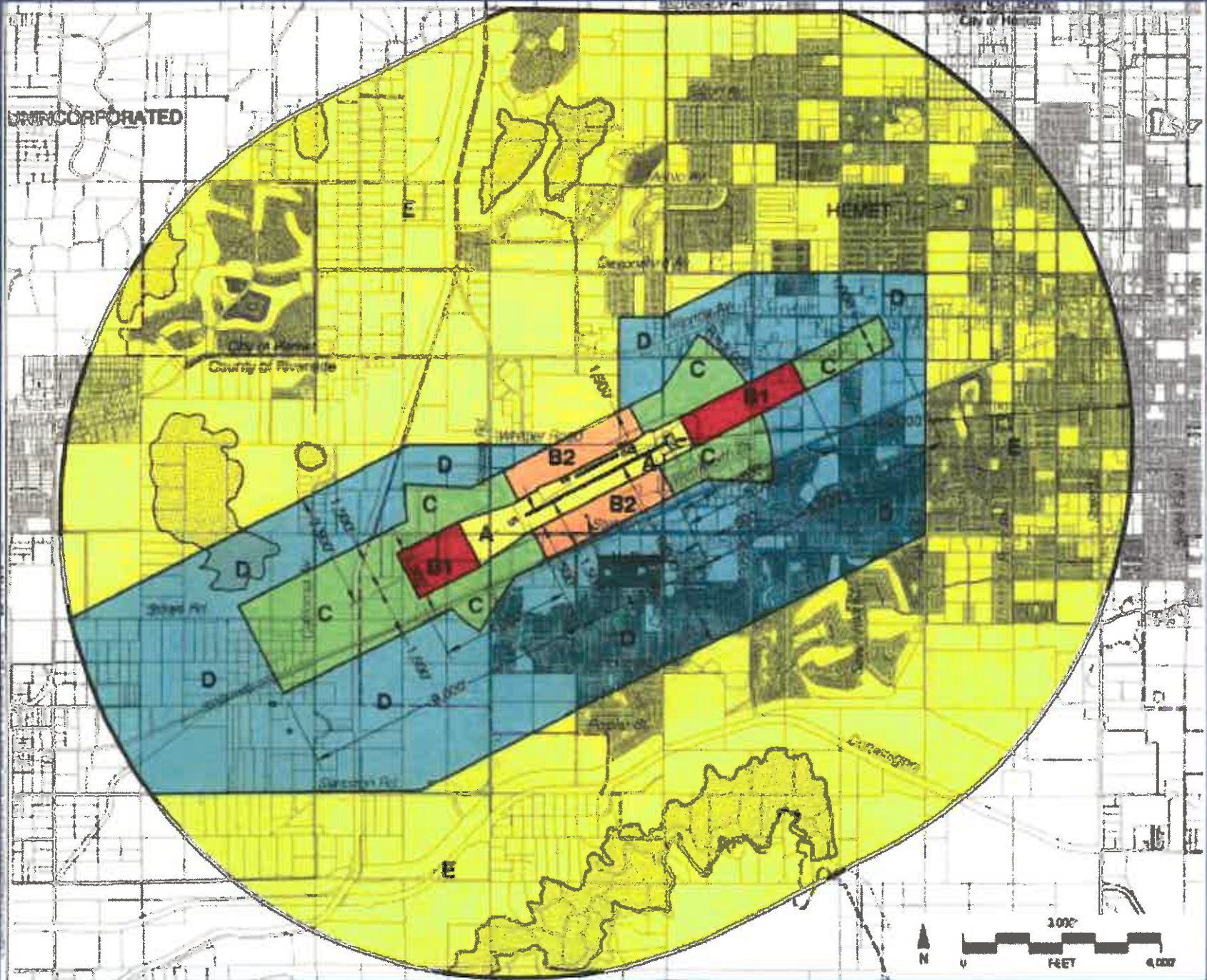
Notes:

- The approach surface for Runway 23 (west end of runway) is adjusted to reflect the proposed use of a displaced threshold and declared distances for the future extended runway.
- The horizontal and conical surfaces are based on the proposed future end of the runway.
- See Policy HR-1 for further explanation and applicable airspace protection policies.

Riverside County
 Airport Land Use Commission
Hemet-Ryan Airport
Land Use Compatibility Plan
 (October 2014 Draft)

Map HR-2
Airport Airspace Plan
 Hemet-Ryan Airport

Source: Hemet-Ryan Airport Master Plan, 2011

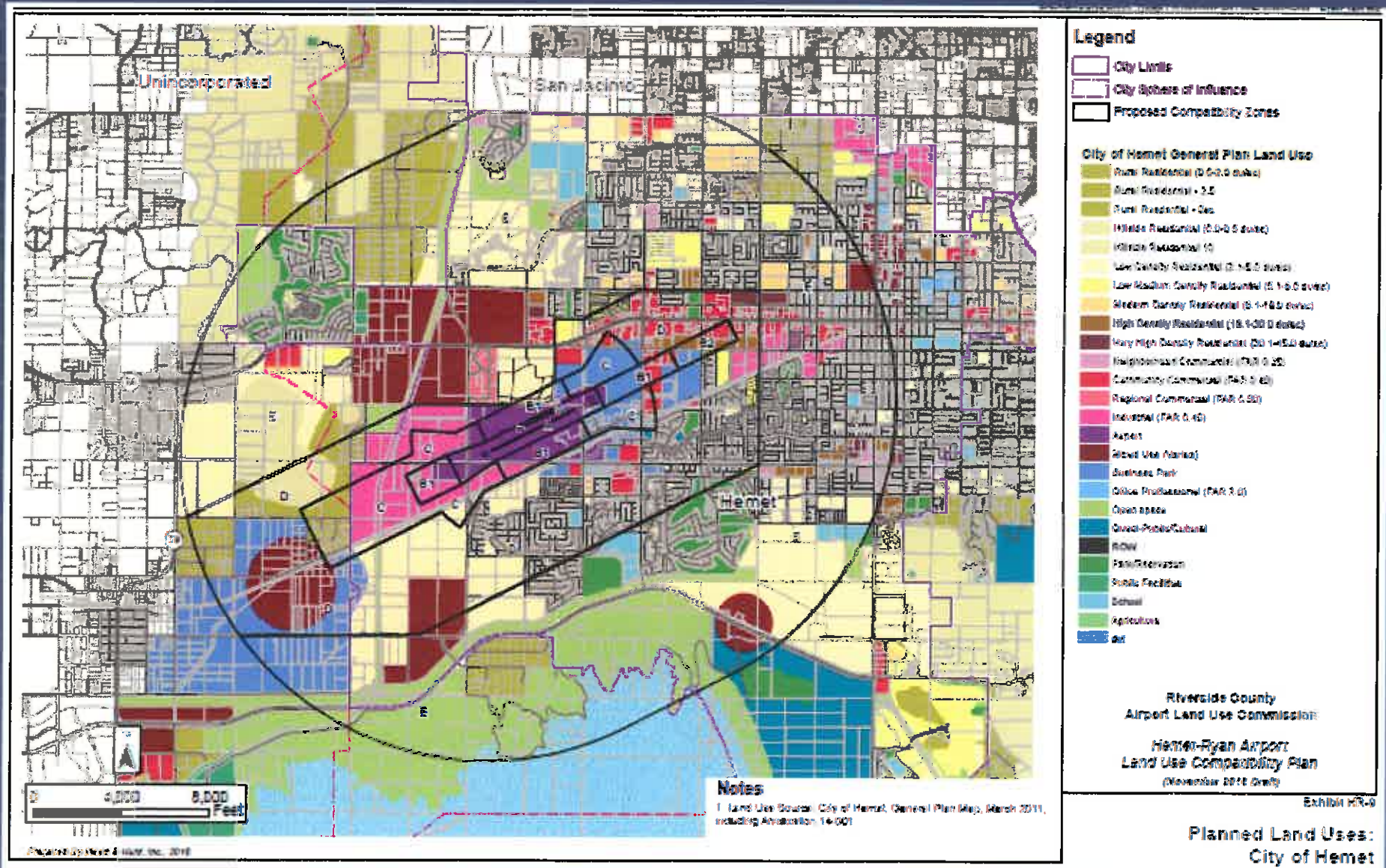


COMPATIBILITY ZONES

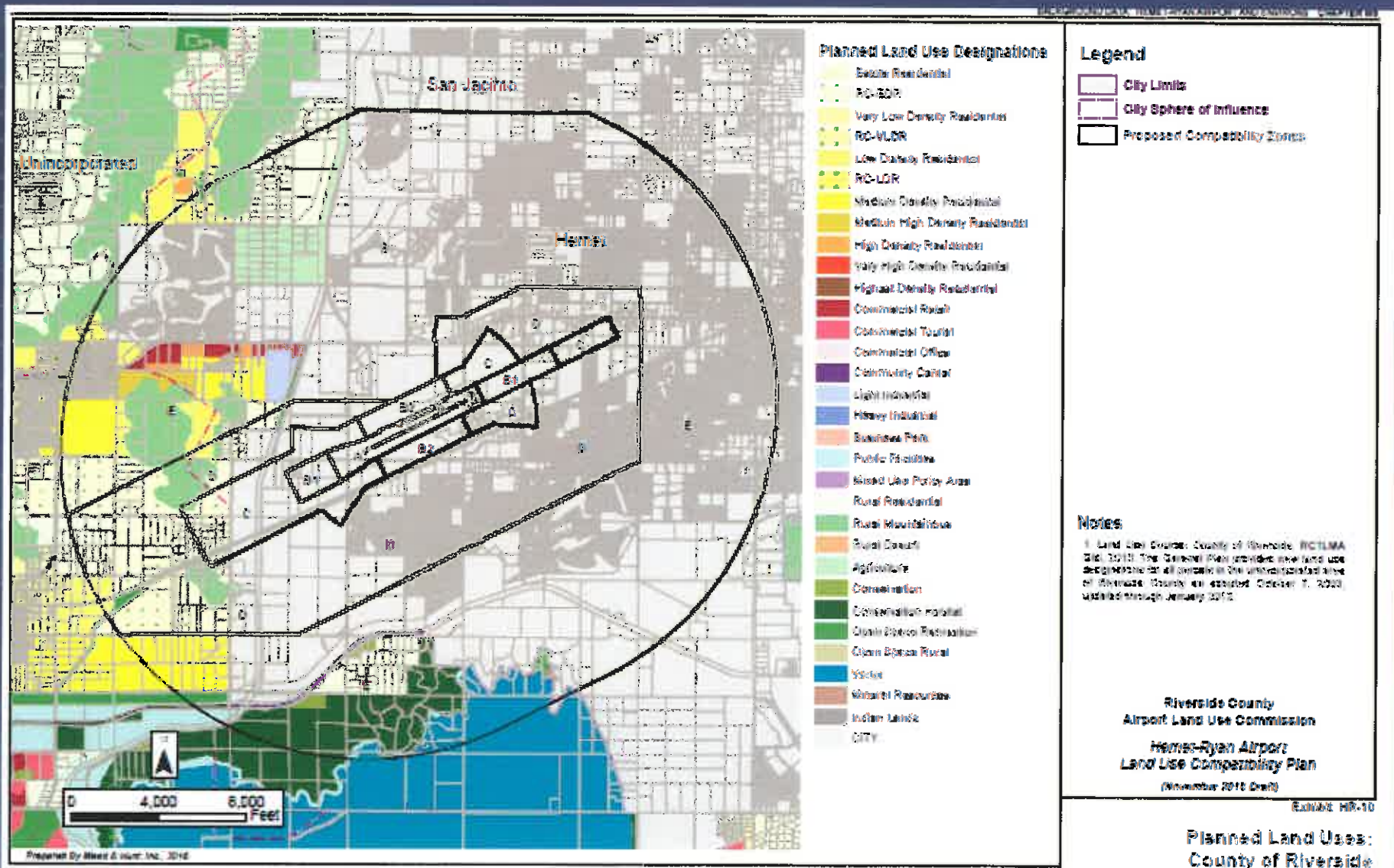
Airport Zone		Residential Density	Prohibited Uses	Open Land
A	Runway Protection Zone	0	All structures and assemblages of people	NA
B1	Inner Zone	1 du/20 acre	Multiple uses	30%
B2	Adjacent to Runway	1 du/10 acre	Multiple uses	0
C	Extended Approach/Departure	1 du/5 acre	Schools, day care, hospitals, nursing homes, libraries, buildings over 3 stories	20%
D	Traffic/Noise Patterns	1 du/2.5 ac OR Greater than 4.5 du/ac	Noise sensitive uses Hazards to flight	10%
E	Airport Environs	No limit		0

	Maximum Non-Residential Intensity (People/Acre)			
Compatibility Zone	Caltrans Handbook	County Plan Single Acre	Proposed HR Single Acre	With Bonus
A	0 (no change)	0	0 (no change)	0 (no change)
B1	40	50	80	104
B2	100 (no change)	200	300	390
C	100	150	250	325
D	200	300	800	1,040
E	No Limit (no change)	No Limit	No Limit (no change)	No Limit (no change)

Proposed Compatibility Zone Densities



City of Hemet General Plan Land Uses



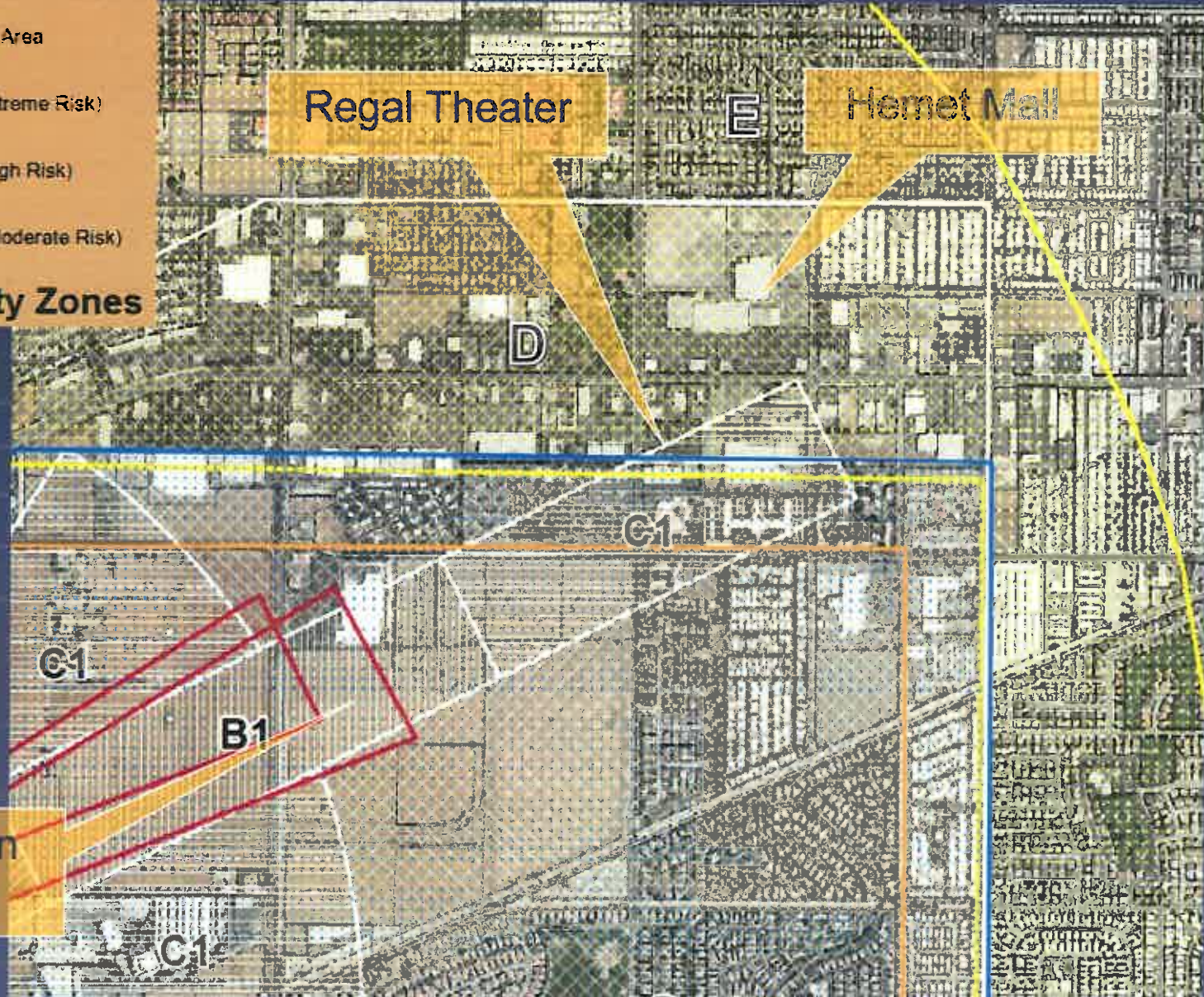
Riverside County General Plan Land Uses



Plan Comparison

-  City Boundary
-  Transition Area
-  Area I (Extreme Risk)
-  Area II (High Risk)
-  Area III (Moderate Risk)

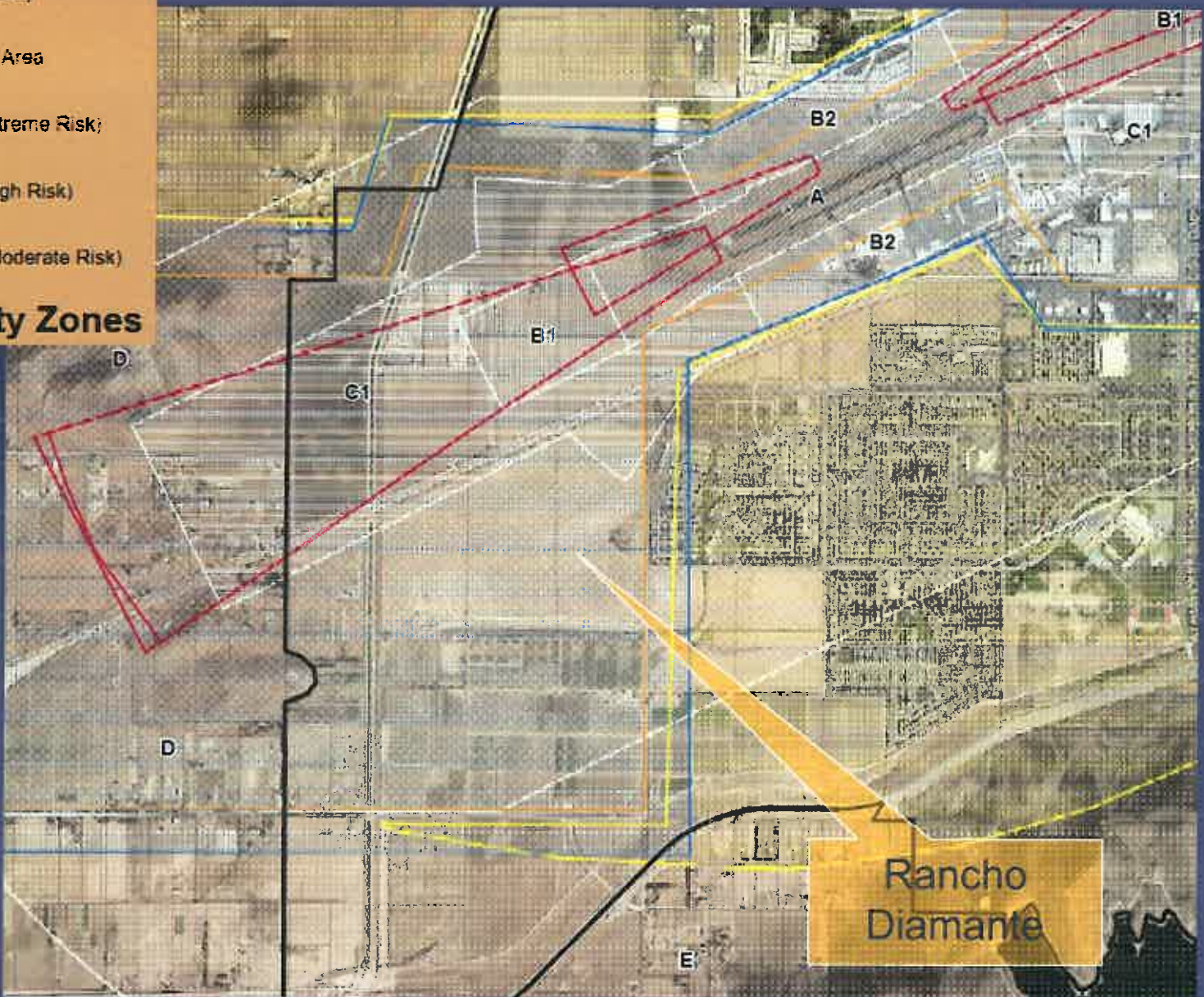
Compatability Zones



Northeast Area

 City Boundary
 Transition Area
 Area I (Extreme Risk)
 Area II (High Risk)
 Area III (Moderate Risk)

Compatability Zones



Southwest Area

Potential Issues

- **Expansion of Compatibility Zone areas to the east (C and D) and impact on infill commercial areas due to new intensity factors**
- **Potential limitation of infill commercial development– although intensity per acre is higher than countywide plan**
- **Impact on approved specific plans and status of previous “over-rides”**
- **Residential development densities in the “D” zone – “the donut hole”**
- **“Open Land Area” requirements in C and D zones**
- **Environmental Analysis of the proposed ALUP**

Requested Changes to Policies

1. No restriction on residential densities in Zone D – should be determined by City's general plan and zoning. Alternatively, set at 3.0 du/net acre and above. Sensitive and large assembly uses discouraged.
2. Allow greater flexibility for infill commercial & industrial intensities in Zone C and D
3. Open land areas to be determined at macro scale (GP & SP) not individual (10+ ac) projects. Designate existing features such as Salt Creek Channel, Stetson, Warren Road, vernal pools, etc. as open land requirement
4. Recognize adopted Specific Plans and projects having previously received "overrides" as being consistent with the ALUP

Property Owner Comments to date

- **Concerns expressed by:**
 - **Benchmark Pacific**
 - **Hemet Valley Mall**
 - **Mr. Sheer – Regal Theaters**

Next Steps

- 1. Obtain Council direction on suggested comments and changes to the ALUP policies**
- 2. Staff to prepare comment letter to ALUC and submit during public review period**
- 3. Staff will continue to meet with ALUC staff to discuss our concerns regarding the plan policies**
- 4. If ALUP is adopted by the ALUC, the City has 180 days to amend our General Plan to be consistent with the Plan.**

PUBLIC COMMENTS, NOVEMBER 7-21, 2016

Guerin, John

From: Rosenthal & Excell <hrge@rosenthalexcell.com>
Sent: Tuesday, November 08, 2016 8:51 AM
To: Guerin, John
Cc: 'Howard Rosenthal'
Subject: Hemet Ryan Airport Land Use Compatibility Plan

John: Re: Hemet Ryan Airport Land Use

Thank you for returning my call yesterday. Here are APN's for which we would like to know how they will be affected

460-020-007 & 008 (Mathias)

456-040-028, 029 & 030 (Rastogi-Wentworth)

445-130-006, 007, 008, 009, 010, 011 (Hemet West)

456-030-015 (REM)

456-030-036, 038, 041 (MEG)

Thank you for your help.

Borghild Wolter

Rosenthal & Excell
1600 E. Florida Ave., Suite 110
Hemet, CA 92544
951-658-2559 Phone
951-658-7690 Fax

Guerin, John

From: azam@sherfamily.com on behalf of Azam Sher <asher@sherington.com>
Sent: Tuesday, November 08, 2016 11:14 AM
To: Guerin, John
Subject: Re: Hemet-Ryan Airport Land Use Compatibility Plan

Many thanks John.

I will review and look forward to speaking tomorrow at 9.

Cheers,

Azam

SHERINGTON

Azam Sher
818-207-8001

On Tue, Nov 8, 2016 at 10:33 AM, Guerin, John <JGUERIN@rctlma.org> wrote:

As requested...The Draft Compatibility Plan map is included in this document.

In order to understand the implications, we recommend going to www.rcaluc.org.

First, click Plans. Go to the fourth document listed, Countywide Policies, and print (33 pages). Then look to the left-hand side while still on the Current Compatibility Plans page, and click 2016 Hemet-Ryan. There you will find Vol. 2 Hemet-Ryan Background and the Initial Study (CEQA document).

Pursuant to the Countywide Policies, adopted in 2004 but not presently applicable to the Hemet-Ryan area, which remains subject to an older (1992) Plan, if we were to adopt the Hemet-Ryan ALUCP without the Additional Compatibility Policies, Zone C would restrict future land use to an average of 75 persons per acre and Zone D to an average of 100 persons per acre. The proposed Hemet-Ryan Plan is more generous, in that its Additional Compatibility Policies allow an average of 100 persons per acre in Zone C and 200 persons per acre in Zone D.

Of course, when plans are updated, there is always the possibility that some properties are subject to greater restrictions than were present in previous plans, while other properties are subject to lesser restrictions than were present in previous plans.

We'll talk tomorrow at 9 AM, but, additionally, I am available for an in-person meeting next week if you would like to go over plans for your property or have in-depth questions.

John J. G. Guerin

Principal Planner

TLMA/ALUC Staff

How are we doing? [Click the Link and tell us.](#)

Guerin, John

From: Rosenthal & Excell <hrge@rosenthalexcell.com>
Sent: Tuesday, November 08, 2016 2:36 PM
To: Guerin, John
Subject: RE: Hemet Ryan Airport Land Use Compatibility Plan
Attachments: 455-13.pdf

John: Thank you for correcting me. Yes, the APN should be 455-130-006-011 (not 445-130).

Borghild Wolter

Rosenthal & Excell
1600 E. Florida Ave., Suite 110
Hemet, CA 92544
951-658-2559 Phone
951-658-7690 Fax

From: Guerin, John [mailto:JGUERIN@rctlma.org]
Sent: Tuesday, November 08, 2016 12:16 PM
To: 'Rosenthal & Excell'; 'Howard Rosenthal'
Cc: Cooper, Ed
Subject: RE: Hemet Ryan Airport Land Use Compatibility Plan

First, APNs 445-130-006 through 445-130-011 are on Le Grande Drive, east of San Jacinto Street, east of Downtown Hemet, not in West Hemet. As such, they are not in the Airport Influence Area and are not affected at all.

The other parcels would definitely be affected in terms of potential intensity of development and open area requirements.

APNs 448-450-011 through 448-450-014 would be within Airport Compatibility Zone D. The City General Plan designation for these parcels is Community Commercial.

APN 456-030-015 would be within Airport Compatibility Zone C. The City General Plan designation for this parcel is Community Commercial. Some commercial uses may be too intense for Zone C.

APN 456-030-036 and 456-030-041 would be split between Compatibility Zones C and D. APN 456-030-038 would be split among Compatibility Zones B1, C, and D. The City General Plan designation is Community Commercial. Some commercial uses may be too intense for Zone C, and most would be too intense for Zone B1. However, these properties are within an area subject to a previous overrule by the City of Hemet. If there is a Development Agreement in effect, please advise.

APN 456-040-028, -029, and -030 would be within Compatibility Zone C. The City General Plan designation for these parcels is Business Park.

APN 460-020-007 and 460-020-008 would be within Airport Compatibility Zone D. The City General Plan designation of 460-020-007 is Business Park. The City General Plan designation of 460-020-008 is Community Commercial.

The Draft Compatibility Plan map and Additional Compatibility Policies are included in the attached document. The Compatibility Plan would affect the potential future use and design of development on vacant parcels and changes of use of existing buildings. (It has no effect on continued use of existing homes or buildings.)

In order to understand the implications, we recommend going to www.rcaluc.org.

First, click Plans. Go to the fourth document listed, Countywide Policies, and print (33 pages). Then look to the left-hand side while still on the Current Compatibility Plans page, and click 2016 Hemet-Ryan. There you will find Vol. 2 Hemet-Ryan Background and the Initial Study (CEQA document).

Pursuant to the Countywide Policies, adopted in 2004 but not presently applicable to the Hemet-Ryan area, which remains subject to an older (1992) Plan, if we were to adopt the Hemet-Ryan ALUCP without the Additional Compatibility Policies, Zone C would restrict future land use to an average of 75 persons per acre and Zone D to an average of 100 persons per acre. The proposed Hemet-Ryan Plan is more generous, in that its Additional Compatibility Policies allow an average of 100 persons per acre in Zone C and 200 persons per acre in Zone D.

Of course, when plans are updated, there is always the possibility that some properties are subject to greater restrictions than were present in previous plans, while other properties are subject to lesser restrictions than were present in previous plans.

Take a look at these documents. If you have any questions, feel free to call me again at (951) 955-0982. Additionally, I am available for an in-person meeting next week if you would like to go over plans for these properties or have in-depth questions. Given that you have or represent a number of affected properties, you (and/or your clients) may want to consider attending the [December 8 public hearing in Perris](#).

For your information, this Plan is being introduced to the Airport Land Use Commission via a presentation from our aviation consultant this Thursday at the Commission's meeting in Riverside. This meeting will not be a public hearing, but it is open to the public. If you attend, you may be able to speak briefly to the consultant after the close of the meeting. If technology is working, you may also be able to view the meeting via live streaming.

John J. G. Guerin
Principal Planner
TLMA/ALUC Staff

[How are we doing? Click the Link and tell us.](#)

From: Rosenthal & Excell [<mailto:hrge@rosenthalexcell.com>]
Sent: Tuesday, November 08, 2016 8:51 AM
To: Guerin, John
Cc: 'Howard Rosenthal'
Subject: Hemet Ryan Airport Land Use Compatibility Plan

John: Re: Hemet Ryan Airport Land Use
Thank you for returning my call yesterday. Here are APN's for which we would like to know how they will be affected
460-020-007 & 008 (Mathias)
456-040-028, 029 & 030 (Rastogi-Wentworth)
445-130-006, 007, 008, 009, 010, 011 (Hemet West)
456-030-015 (REM)
456-030-036, 038, 041 (MEG)
Thank you for your help.

Borghild Wolter
Rosenthal & Excell
1600 E. Florida Ave., Suite 110
Hemet, CA 92544
951-658-2559 Phone
951-658-7690 Fax

Guerin, John

From: Jack Vander Woude <tahiti.tahiti@verizon.net>
Sent: Monday, November 14, 2016 7:48 AM
To: Guerin, John
Subject: Hemet-Ryan

Good morning, John.....Been a while since we last spoke! I can't seem to find on line the Land Use Map re the 2016 Land Use Compatibility Plan.....will appreciate it if you could please point me in the right direction as to where I may be able to view it??

Thanks....and hope all is well with you.

Cordially,

Jack Vander Woude - Principal
THE TAHITI GROUP - Redlands, CA
mail: P.O. Box 11291
San Bernardino, CA 92423
(909) 798-8750
tahiti.tahiti@verizon.net



=====
=====

Guerin, John

From: Santos, Barbara
Sent: Monday, November 14, 2016 12:55 PM
To: Guerin, John
Cc: Rull, Paul
Subject: FW: ALUC : Comment/Question

FYI:

From: socaltga@gmail.com [<mailto:socaltga@gmail.com>]
Sent: Friday, November 11, 2016 1:02 PM
To: Santos, Barbara
Subject: ALUC : Comment/Question

ALUC : Comment/Question

Name	Teresa Acuna
Email	socaltga@gmail.com
Subject	2016 Hemet-Ryan Documents
Message	I am unable to print the draft Vol 1 & Vol 2 documents. I am able to print the other items but not the first 2. We have board of directors meeting pending and need to be able to provide these documents. Plz advise.

Guerin, John

From: Sordello, Tony J@DOT <tony.sordello@dot.ca.gov>
Sent: Wednesday, November 16, 2016 3:05 PM
To: Guerin, John
Subject: Draft ALUCP

Importance: High

Hi John,

I have down loaded the November 2016 draft Hemet – Ryan ALUCP chapter 3 and 5, those are the only chapters correct?

Also, do you have a list of what was changed from the March 2016 drafts? I have reviewed the March 2016 drafts and it would save me a lot of time to know what has changed. If you do not have a list can you let me know if the maps including HR-1 Compatibility Map have been changed from the March 2016 version?

Thanks



Tony Sordello
CALTRANS Division of Aeronautics
Office of Aviation Planning
(916) 654-5203
tony.sordello@dot.ca.gov
MS 40 P.O. Box 942874
Sacramento, CA 94274-0001

Caltrans Mission: Provide a safe, sustainable, integrated, and efficient transportation system to enhance California's economy and livability.

Caltrans Vision: A performance-driven, transparent, and accountable organization that values its people, resources and partners, and meets new challenges through leadership, innovation, and teamwork

Guerin, John

From: Sordello, Tony J@DOT <tony.sordello@dot.ca.gov>
Sent: Thursday, November 17, 2016 6:51 AM
To: Guerin, John
Subject: RE: Draft ALUCP
Attachments: TS-HMT.Chap 3.Policies.2016-10-17 dft.pdf; HMT.Chap 3.Policies.2016-10-17 dft.pdf

Importance: High

John,

Thank you for the information. I'm sorry I meant October 2016. Attached are the ones I have reviewed and would like to know if any change have been made to them especially the maps?



Tony Sordello
CALTRANS Division of Aeronautics
Office of Aviation Planning
(916) 654-5203
tony.sordello@dot.ca.gov
MS 40 P.O. Box 942874
Sacramento, CA 94274-0001

Caltrans Mission: Provide a safe, sustainable, integrated, and efficient transportation system to enhance California's economy and livability.

Caltrans Vision: A performance-driven, transparent, and accountable organization that values its people, resources and partners, and meets new challenges through leadership, innovation, and teamwork

From: Guerin, John [mailto:JGUERIN@rctlma.org]
Sent: Wednesday, November 16, 2016 5:46 PM
To: Sordello, Tony J@DOT <tony.sordello@dot.ca.gov>
Cc: Cooper, Ed <ECOOPER@rctlma.org>; Ken Brody (ken.brody@meadhunt.com) <ken.brody@meadhunt.com>
Subject: RE: Draft ALUCP

March 2016 is not ringing any bells. Earlier this year, I sent you the version dated December 2013, which was the latest draft at that time.

Looking at the Chapter 3 document, sections 1.5, 2.2, 2.3, and 2.4 are new/added in comparison to the 2013 draft. The other major changes relate to map revisions to reflect annexation by the City of Hemet and exclusion from the AIA of a strip of land in the City of San Jacinto.

The following changes are in reference to the December 2013 text layout.

Overview page – The only change was a change in date from December 2013 to November 2016.

Section 1.1. In the second sentence on Line 3, changed “2013” to “2016.” In the third sentence on Line 3, changed “August 2012” to “May 2011.” In the fifth sentence on Line 6, added “(ALP)” following the phrase “Airport Layout Plan.” The sixth sentence was deleted in its entirety and replaced with the following: “In September 2015, the FAA agreed that the runway configuration shown on the May 2013 draft ALP is the one that will be pursued in the full update of the ALP drawing set scheduled for 2017. Subsequently, in October 2015, the Caltrans Division of Aeronautics accepted use of this ALP as the basis for the Hemet-Ryan ALUCP.”

Section 1.2. In the second sentence of the first paragraph on Line 2, changed “calls for” to “and ALP propose.” In the third sentence of the second paragraph on Line 3, changed “master plan calls” to “draft Airport Master Plan and ALP call.”

Section 1.3. In the first sentence on Line 2 and the third sentence on Line 3, changed “2011” to “2010.” In the second sentence on Line 2, changed “2031” to “2036.”

Section 1.4. The second sentence was amended to read as follows (new in bold): “**With one exception, the outer edge of the FAR Part 77 conical zone (previously “surface”) therefore is used to set the airport influence boundary established in this Compatibility Plan and shown on Map HR-1.**” A new sentence was inserted reading as follows: “**That exception is to exclude from the airport influence area the small section of the conical zone that extends into the City of San Jacinto north of the airport.**” In the following sentence, added the phrase “as well” following the word “Note,” changed “surfaces” to “zones” and added the phrase “depicted in Map HR-2” following the phrase “Part 77 zones,” inserted the word “proposed” prior to the word “future,” and changed “proposed” prior to the phrase “use of declared distances” to “anticipated.” A final sentence was added to the paragraph stating as follows: “**The airport influence area also encompasses the forecast noise contours shown in Map HR-3.**”

Section 1.5 NEW

Section 2.1(b) was modified to read as follows (new in bold): “**Except as modified by other policies in this section, all other provisions of Table 2A apply to Hemet-Ryan Airport.**”

Section 2.2 NEW (added to limit potential displacement of future commercial uses)

Section 2.3 NEW (added to limit potential displacement of future residential uses)

Section 2.4 NEW

Please be aware that there is a potential for additional modifications by the Commission through the public hearing process.

Maps – The only change to the map relative to the AIA and Compatibility Zone boundaries was to limit the northerly boundary, which is now proposed to end at Esplanade Avenue so as not to extend into a small area of the City of San Jacinto, which has not been part of the process. In a recent meeting, the City of Hemet advised us of an annexation of land into the City. All maps were amended to depict the new City boundaries. The City’s sphere of influence boundary was also added to Map HR-1, and the date changed to November 2016.

Map HR-2. The red dashed lines were added, as were all of the notes.

Future Noise Impacts - No change except addition of new City boundaries.

From: Sordello, Tony J@DOT [<mailto:tony.sordello@dot.ca.gov>]

Sent: Wednesday, November 16, 2016 3:05 PM

To: Guerin, John
Subject: Draft ALUCP
Importance: High

Hi John,

I have down loaded the November 2016 draft Hemet – Ryan ALUCP chapter 3 and 5, those are the only chapters correct?

Also, do you have a list of what was changed from the March 2016 drafts? I have reviewed the March 2016 drafts and it would save me a lot of time to know what has changed. If you do not have a list can you let me know if the maps including HR-1 Compatibility Map have been changed from the March 2016 version?

Thanks



Tony Sordello
CALTRANS Division of Aeronautics
Office of Aviation Planning
(916) 654-5203
tony.sordello@dot.ca.gov
MS 40 P.O. Box 942874
Sacramento, CA 94274-0001

Caltrans Mission: *Provide a safe, sustainable, integrated, and efficient transportation system to enhance California's economy and livability.*

Caltrans Vision: *A performance-driven, transparent, and accountable organization that values its people, resources and partners, and meets new challenges through leadership, innovation, and teamwork*

2016 HEMET-RYAN AIRPORT LAND USE COMPATIBILITY PLAN

NOTICE OF AVAILABILITY OF INITIAL STUDY AND NEGATIVE DECLARATION NOTICE OF INTENT TO ADOPT A NEGATIVE DECLARATION NOTICE OF PUBLIC HEARING

The Riverside County Airport Land Use Commission ("Commission") intends to adopt a Negative Declaration, prepared pursuant to the California Environmental Quality Act ("CEQA") (Pub. Resources Code §21000 *et seq.*), for the proposed 2016 Hemet-Ryan Airport Land Use Compatibility Plan ("2016 HRALUCP").

Brief Project Description: The 2016 HRALUCP contains policies for determining the consistency of future development within the Hemet-Ryan Airport Influence Area ("AIA") with the objectives set forth in the State Aeronautics Act (Pub. Util. Code, §§21670-21679.5). These objectives seek to protect public health, safety, and welfare by providing for locally-established land use measures that provide for the orderly expansion of public use airports and minimize the public's exposure to excessive noise and safety hazards within areas around those airports, to the extent that such areas are not already devoted to incompatible uses. The degree of land use restrictiveness is greatest closest to the runway and in areas in line with the runway, and relatively limited in the outer areas.

The 2016 HRALUCP would modify the boundaries of the AIA, as located in the City of Hemet and unincorporated Riverside County. Further, adoption of the 2016 HRALUCP would result in the geographic area encompassed by the AIA being subject to the provisions of the 2004 Riverside County Airport Land Use Compatibility Plan ("County ALUCP"), except as modified by proposed "Additional Compatibility Policies" for this Airport. The "Additional Compatibility Policies" ease the restrictiveness of the County ALUCP's density and intensity limitations.

As noted above, the 2016 HRALUCP applies only to future development, and not to existing land uses. The HRALUCP does not propose the development or acquisition of land. Additionally, neither the HRALUCP nor the Commission has any jurisdiction over the operation of Hemet-Ryan Airport or aircraft in flight.

Document Availability: The proposed 2016 HRALUCP, Negative Declaration and supporting Initial Study, and all documents referred to in the Initial Study are available for public inspection and review upon request to John J. G. Guerin, Principal Planner (contact information below), at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California, 92501, Monday through Friday, between the hours of 9:00 a.m. and 5:00 p.m. Copies of the proposed 2016 HRALUCP, Negative Declaration and Initial Study also are available for review on the Commission's website at www.rcaluc.org.

Public Review Period: The Commission will receive public comments on the proposed Negative Declaration and Initial Study for a 30-day period, **beginning November 7, 2016, and concluding December 7, 2016**, at 5:00 p.m., and at the public hearing referenced below. Written comments must be submitted to John J. G. Guerin, Principal Planner, Riverside County Airport Land Use Commission, Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California, 92501, or may be e-mailed to jguerin@rctlma.org.

Public Hearing: The Commission will hold a public hearing to consider the proposed 2016 HRALUCP, Negative Declaration and Initial Study on **December 8, 2016 at 9:30 A.M., at the Eastern Municipal Water District Board Room located at 2270 Trumble Road, Perris, California 92570**. (This is the hearing venue only; do not send written comments to this address.) In compliance with the Americans with Disabilities Act, if any accommodations are needed, please contact Barbara Santos at (951) 955-5132 or e-mail at basantos@rctlma.org. Requests should be made at least 48 hours (or as early as possible) prior to the hearing.

Per Public Resources Code Section 21177, no action or proceeding may be brought under CEQA to challenge the Commission's adoption of the Negative Declaration, or approval of the 2016 HRALUCP, unless the alleged grounds for noncompliance were presented to the Commission either orally or in writing during the public comment period or prior to the close of the public hearing on the project before the issuance of the notice of determination.

For additional information, contact John Guerin at (951) 955-0982 or ALUC staff at (951) 955-5132.

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

STAFF REPORT

ADMINISTRATIVE ITEMS

5.1 Director's Approvals.

- A. During the period of October 25, 2016 through November 23, 2016, as authorized pursuant to Section 1.5.2(d) of the 2004 Riverside County Airport Land Use Compatibility Plan, ALUC Director Ed Cooper reviewed seven non-legislative cases within Compatibility Zones D and E of Airport Influence Areas not associated with legislative cases and issued determinations of consistency.

ZAP1065BD16 (Bermuda Dunes, Zone E) pertains to City of La Quinta Case No. SDP 2016-0007 (Site Development Permit), a proposal to demolish an existing golf course building and construct a new single-story 5,544 square foot medical office building on a 1.3-acre parcel located on the southeast corner of Jefferson Street and Westward Ho Drive, extending southerly to Palm Circle Drive. The elevation of Runway 10-28 at its easterly terminus is approximately 49 feet above mean sea level (AMSL). At a distance of 11,250 feet from the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with top of roof exceeding 161 feet AMSL. The project site elevation is 60 feet AMSL, and the maximum height of the proposed building is 19 feet, for a maximum top point elevation of 79 feet AMSL. Therefore, FAA OES review for height/elevation reasons was not required. ALUC Director Ed Cooper issued a determination of consistency for this project on November 2, 2016.

ZAP1223MA16 (March Air Reserve Base/Inland Port Airport, Zone E) pertains to City of Riverside Case Nos. P16-0082 (Conditional Use Permit) and P16-0083 (Design Review), proposing demolition of an existing restaurant building, installation of six new fuel pumps (with a 3,268 square foot fueling canopy area) as an addition to an existing gasoline service station, and establishment of a 1,960 square foot self-serve car wash on a 0.88-acre site that also includes an existing convenience store located at 1340-1360 West Blaine Street, westerly of Iowa Avenue and northeasterly of the Interstate 215 Freeway. The site elevation is more than 500 feet lower than the elevation of March's Runway 14-32 at its northerly terminus (1,535 feet above mean sea level). The site is located less than 20,000 feet from Runway 6-24 at Flabob Airport with an elevation of 768 feet above mean sea level. However, as the length of that runway does not exceed 3,200 feet, the relevant horizontal distance for notice purposes is 10,000 feet. This site is located 19,500 feet from that runway. Therefore, Federal Aviation Administration Obstruction Evaluation Service review for height/elevation reasons was not required. ALUC Director Ed Cooper issued a determination of consistency for this project on November 4, 2016.

ZAP1022BA16 (Banning, Zone E) pertains to City of Banning Case Nos. CUP16-8002 (Conditional Use Permit) and DR16-7003 (Design Review), proposing construction of a 10,500 square foot building with a height of 24 feet for indoor storage of recreational vehicles and contractors' equipment and to operate an equipment rental and RV storage yard on a 4.5-acre parcel located northerly of Charles Street, southerly of Barbour Street, easterly of Driftwood Circle, and westerly of Hathaway Street. The elevation of Runway 8-26 at Banning Municipal Airport is approximately 2,219 feet above mean sea level (AMSL). At a distance

of 1,969 feet from the runway to the project site, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with top of roof exceeding 2,239 feet AMSL. The existing site elevation is approximately 2,205 feet AMSL. The proposed maximum height of the building is 24 feet, for an approximate total maximum elevation of 2,229 feet AMSL. Therefore, FAA OES review for height/elevation reasons was not required. ALUC Director Ed Cooper issued a determination of consistency for this project on November 7, 2016.

ZAP1226MA16 (March, Zone E) pertains to City of Riverside Case No. P16-0766 (Conditional Use Permit), a proposal to establish a private university within approximately 14,000 square feet of an existing 5-story office building on a 2.81-acre parcel with an address of 1325 Spruce Street, located at the northwest corner of Iowa Avenue and Spruce Street. Nonresidential intensity is not limited in Compatibility Zone E. No change to building height or construction of additional structures is proposed. Therefore, Federal Aviation Administration Obstruction Evaluation Service review for height/elevation reasons was not required. ALUC Director Ed Cooper issued a determination of consistency for this project on November 8, 2016.

ZAP1224MA16 (March, Zone D) pertains to March Joint Powers Authority Case No. 16-01 (Plot Plan), a proposal to construct a two-story 109,141 square foot acute care hospital with 156 beds on a 7.1-acre parcel located northerly of N Street, westerly of 6th Street, easterly of Reservation Drive, and southerly of Myers Drive, northeasterly of the March runway within the land use jurisdiction of March Joint Powers Authority. Within Compatibility Zone D of the March Air Reserve Base/Inland Port Airport Influence Area, nonresidential intensity is not restricted. Although the Part 77 notice threshold was not breached, the applicant submitted the project to the Federal Aviation Administration Obstruction Evaluation Service (FAA OES) for review and received a FAA Determination of No Hazard to Air Navigation letter dated January 7, 2016. The conditions of that letter were incorporated into ALUC's recommended conditions. ALUC Director issued a determination of consistency for this project on November 9, 2016.

ZAP1225MA16 (March, Zone E) pertains to City of Riverside Case Nos. P16-0732 (Design Review) and P16-0731 (Tentative Parcel Map). P16-0732 is a proposal to construct five warehouse buildings ranging in size from 10,000 square feet to 13,850 square feet on 4.05 acres within two Assessor's parcels located on the easterly side of Rustin Avenue, southerly of its intersection with Marlborough Avenue. P16-0731 is a proposal to subdivide the 4.05-acre project site into five parcels. The site elevation is more than 500 feet lower than the elevation of March's Runway 14-32 at its northerly terminus (1,535 feet above mean sea level), and the site is located more than 20,000 feet from the runways at Riverside Municipal Airport and Flabob Airport. Therefore, Federal Aviation Administration Obstruction Evaluation Service review for height/elevation reasons was not required. ALUC Director Ed Cooper issued a determination of consistency for this project on November 10, 2016.

ZAP1227MA16 (March, Zone D) pertains to City of Riverside Case No. P16-0671 (Design Review), a proposal to construct a 23,290 square foot, two-story medical office building on a 1.62-acre parcel located at 18872 Van Buren Boulevard (easterly of Prairie Way and westerly of Wood Road). The applicant is also proposing a tentative map for condominium purposes, which does not affect ALUC review since it would relate to the same structure analyzed through the Design Review proposal. Within Compatibility Zone D of the March Air Reserve Base/Inland Port Airport Influence Area, nonresidential intensity is not restricted. The elevation of March's Runway 14-32 at its northerly terminus is 1,535 feet above mean sea level (AMSL). At a distance of 18,560 feet from the runway to the project site, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with top of roof exceeding 1,720 feet AMSL. The site elevation is approximately 1,636 feet AMSL, and the proposed building height is 40 feet, resulting in a maximum top point elevation of 1,676 feet AMSL. Therefore, FAA

OES review for height/elevation reasons was not required. ALUC Director Ed Cooper issued a determination of consistency for this project on November 14, 2016.

- B. Additionally, ALUC Director Ed Cooper reviewed the following legislative cases within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area pursuant to ALUC Resolution No. 2015-01 and issued determinations of consistency.

ZAP1219MA16 includes two legislative cases and one non-legislative case in unincorporated Riverside County. General Plan Amendment No. 1183 (GPA01183) is a proposal to amend the General Plan (Lakeview/Nuevo Area Plan) designation of 18.36 acres located southerly of Central Avenue, northwesterly of Porter Street, and easterly of Palomar Road from Rural Community: Low Density Residential (RC-LDR) to Community Development: Medium Density Residential (MDR). Change of Zone Case No. 7919 (CZ07919) is a proposal to change the zoning of this area from Residential Agricultural (R-A) to Planned Residential (R-4). The non-legislative case is Tentative Tract Map No. 37134, a proposal to divide this same area into 75 single-family residential lots. Residential density is not limited in Compatibility Zone E. The site elevation is 1,448 feet above mean sea level (AMSL). With a maximum building height of 40 feet, structures would not exceed the elevation of the runway at March Air Reserve Base. The site is actually closer to Perris Valley Airport, at a distance of 19,281 feet. The elevation of the runway at Perris Valley Airport is 1,413 feet AMSL. At a distance of 19,281 feet from the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with top of roof exceeding 1,605 feet AMSL. As the maximum top point elevation at this site would not exceed 1,488 feet AMSL, FAA OES review for height/elevation reasons was not required. ALUC Director Ed Cooper issued determinations of consistency for these projects on November 3, 2016.

Copies of these consistency letters and background documents are attached, for the Commission's information.

- 5.2 Specific Delegation of Authority: Contractor's Storage Yard in March Zone C2. The applicant for ZAP1229MA16 (County of Riverside Case No. PP26118) is requesting that the Commission delegate to the ALUC Director the ability to render a consistency determination for this project, a proposal to establish a contractor's storage yard with a 528 square foot mobile office on a 0.89-acre site located on the northerly side of Peregrine Way, westerly of Harvill Avenue and southerly of Oleander Avenue in the unincorporated community of Mead Valley, within Compatibility Zone C2 of the March Air Reserve Base/Inland Port Airport Influence Area. The project representative advises that he was not informed of the requirement for ALUC review until after the deadline for submittal for today's hearing had passed, and staff was not able to include this case on today's agenda, which was already full. The applicant has little time left on his lease of property at his present business location. Staff recommends that the Commission consider the applicant's request for a specific delegation of authority to the ALUC Director to render a consistency determination for this project.

Y:\ALUC\ALUC Administrative Items\Admin. 2016\ADmin Item 12-08-16.doc

Redlands Consultants & Associates

Engineering, Planning, Surveying, Construction Management

Item
5.2

11.09.2016

John Guerin

Riverside County

Airport Land Use Commission

Re: PP26118, ZAP1229MA16

John,

I am sending this letter at the request of the owner for the property with the referenced projects above. We are requesting to be placed on the December commission hearing. We know we have passed the deadline date for the application. However, we did not know about the ALUC until last week, per the attached email from the County Planner, Desiree.

The project is only a contractor's yard with a 528 SF mobile office. Lighting will only be from flood lights placed on the northwest and northeast corners of the Mobile office directed northerly and northwesterly. The lights will also be directed down.

The client searched for a considerable time for a contractor's yard property as his lease on his existing property expires soon. Whatever you can do to place this project on the soonest commission would be appreciated.

Thank you,

Khaled (Kal) Farah



**AIRPORT LAND USE COMMISSION
RIVERSIDE COUNTY**

*Item
5.1*

November 2, 2016

CHAIR
Simon Housman
Rancho Mirage
VICE CHAIRMAN
Rod Ballance
Riverside

Mr. Carlos Flores, Project Planner
City of La Quinta Planning Department
78-495 Calle Tampico
La Quinta CA 92253

COMMISSIONERS

Arthur Butler
Riverside

John Lyon
Riverside

Glen Holmes
Hemet

Steve Manos
Lake Elsinore

Russell Betts
Desert Hot Springs

STAFF

Director
Ed Cooper

John Guerin
Paul Rull
Barbara Santos

County Administrative Center
4080 Lemon St., 14th Floor
Riverside, CA 92501
(951) 955-5132

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –
DIRECTOR’S DETERMINATION**

File No.: ZAP1065BD16
Related File No.: SDP 2016-0007 (Site Development Plan)
APN: 600-070-012

Dear Mr. Flores:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Section 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed City of La Quinta Case No. SDP 2016-0007 (Site Development Plan), a proposal to demolish an existing golf course building and construct a new single-story 5,544 square foot medical office building on a 1.3 acre parcel located on the southeast corner of Jefferson Street and Westward Ho Drive, extending southerly to Palm Circle Drive, in the City of La Quinta.

The site is located within Airport Compatibility Zone E of the Bermuda Dunes Airport Influence Area (AIA). Compatibility Zone E does not limit nonresidential intensity.

The project is located outside the 55 CNEL noise contour for the Bermuda Dunes Airport.

The elevation of Runway 10-28 at its easterly terminus is approximately 49 feet above mean sea level (AMSL). At a distance of approximately 11,250 feet from the runway, FAA review would be required for any structures with top of roof exceeding 161 feet AMSL. The project site elevation is 60 feet AMSL, and the maximum height of the proposed building is 19 feet, for a maximum top point elevation of 79 feet AMSL. Therefore, Federal Aviation Administration (FAA) obstruction evaluation review for height/elevation reasons is not required for the building.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2004 Bermuda Dunes Airport Land Use Compatibility Plan, subject to the following conditions:

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded as to prevent either the spillage of lumens or reflection into the sky.

www.rcaluc.org

AIRPORT LAND USE COMMISSION

2. The following uses are prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area.
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached notice shall be provided to all potential purchasers of the property and tenants of the existing and proposed buildings thereon.
4. Any new detention basin(s) on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.

If you have any questions, please contact Paul Rull, Urban Regional Planner IV, at (951) 955-6893, or John Guerin, Principal Planner, at (951) 955-0982.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Edward C. Cooper, Director

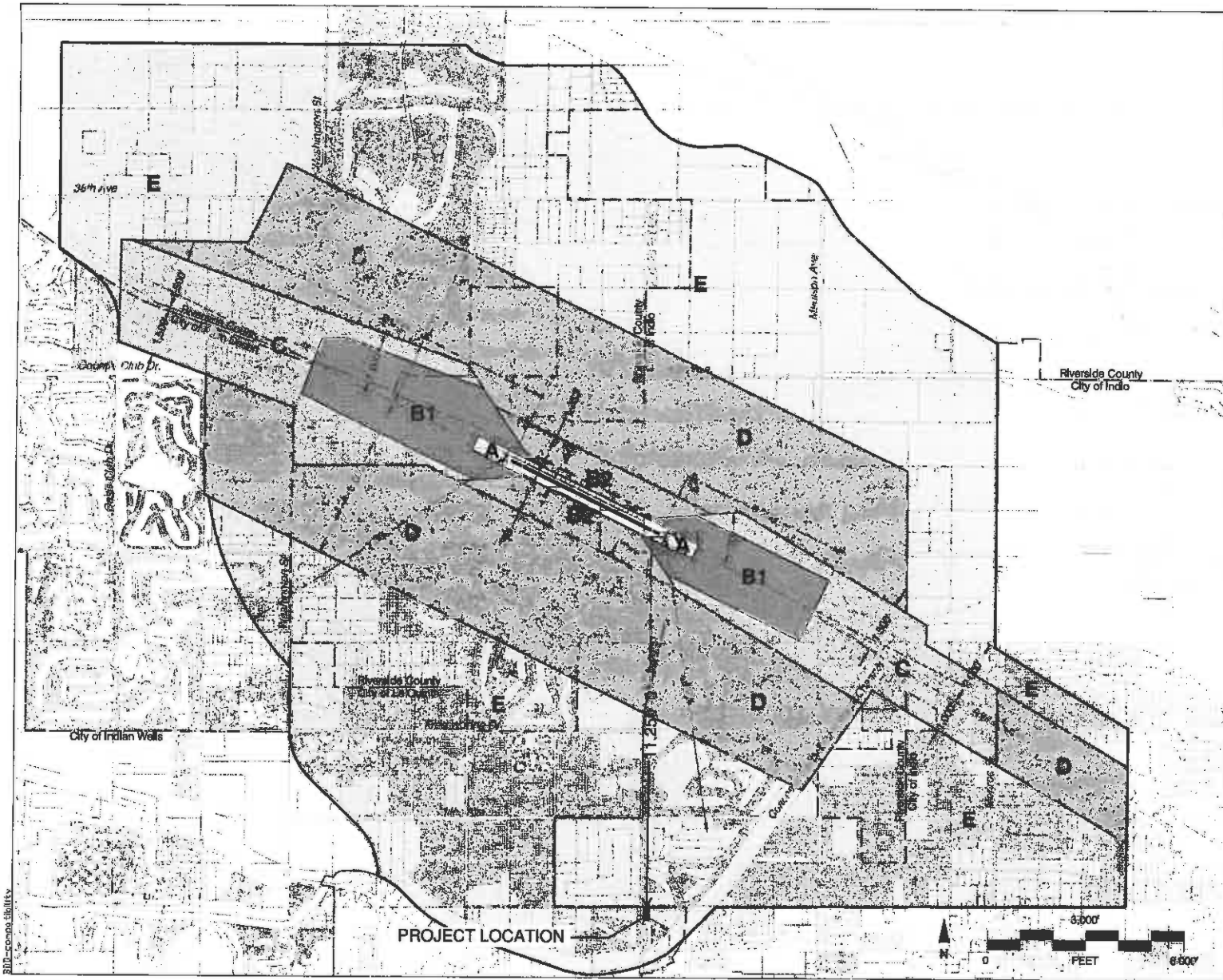
PR
Attachments: Notice of Airport in Vicinity

cc: Prest Vuksic Architects (representative)
EJ La Quinta LLC Stephenson/Timothy Jochen and Lee Erwin (property owner)
Robert Berriman, Manager, Bermuda Dunes Executive Airport
ALUC Case File

Y:\AIRPORT CASE FILES\Bermuda Dunes\ZAP1065BD16\ZAP1065BD16LTR.doc

NOTICE OF AIRPORT IN VICINITY

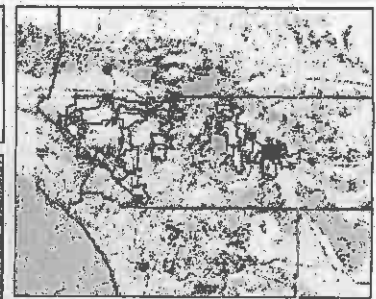
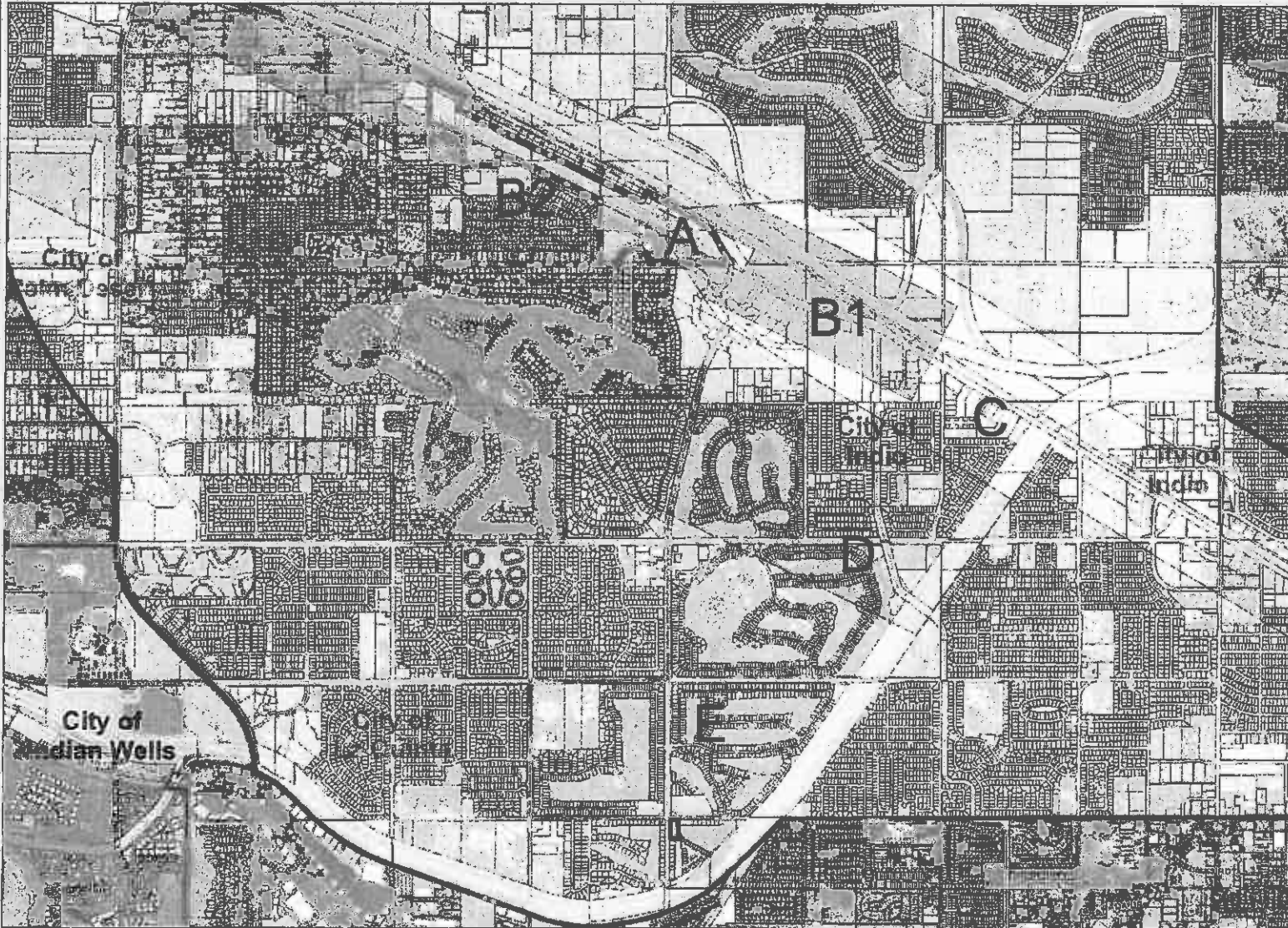
This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)





























PROJECT IN RELATIONSHIP TO
AIRPORT APPROX. 11,250 FT

PROJECT ADDRESS
46080 Jefferson Street
La Quinta, CA. 92253

My Map



Legend

-  Airports
-  AIA
- Airport Compatibility**
-  OTHER ZONE
-  A
-  A-EXC1
-  B1
-  B1-APZ I
-  B1-APZ I-EXC1
-  B1-APZ II
-  B1-APZ II-EXC1
-  B1-EXC1
-  B2
-  B2-EXC1
-  C
-  C1
-  C1-EXC1
-  C1-EXC3
-  C1-EXC4
-  C1-HIGHT
-  C2
-  C2-EXC1
-  C2-EXC2
-  C2-EXC3
-  C2-EXC5
-  C2-EXC6
-  C2-HIGHT



0 3,578 7,155 Feet



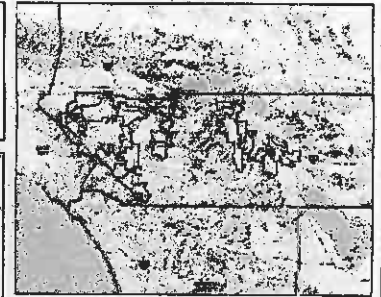
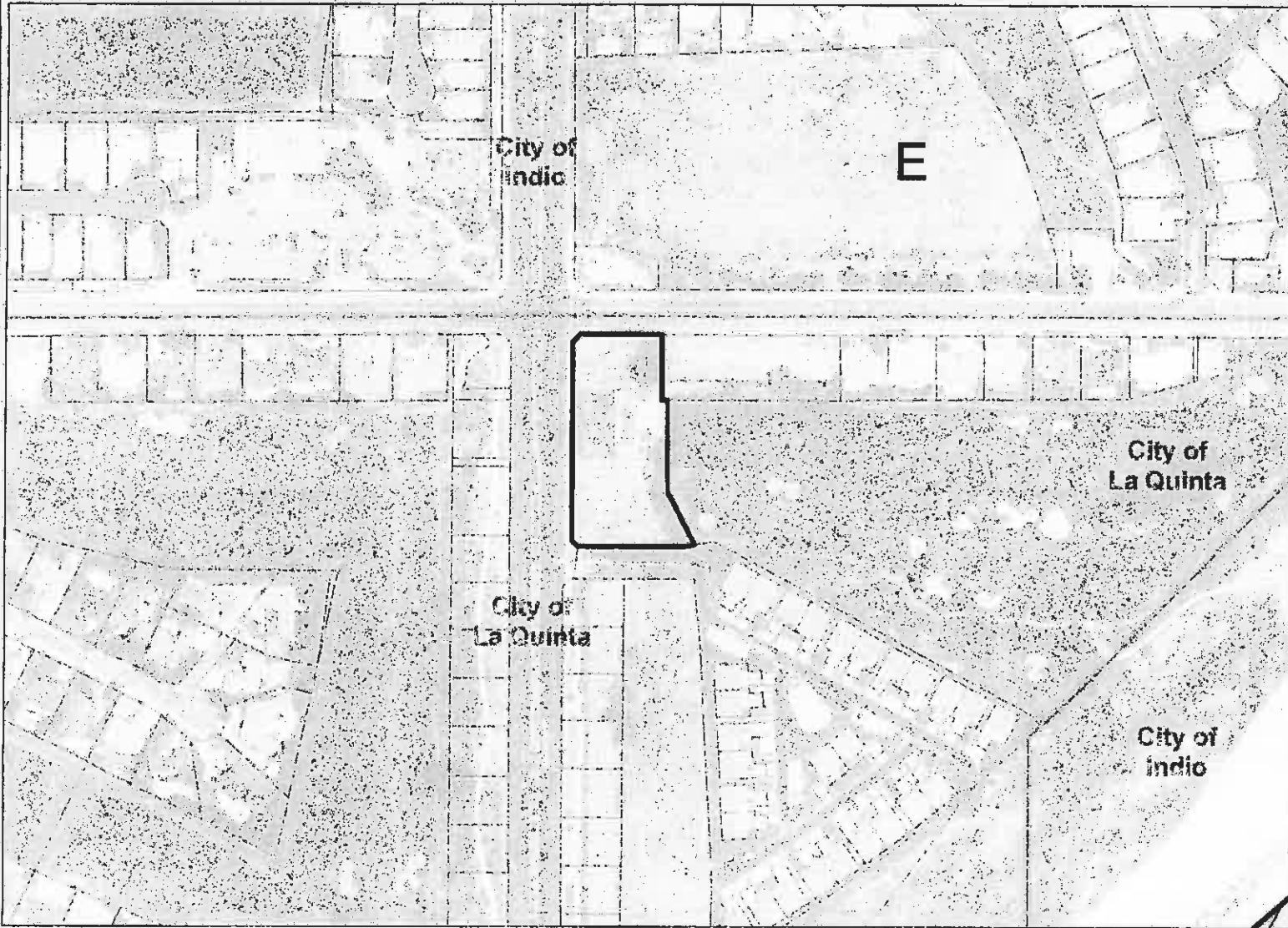
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/19/2016 9:01:06 AM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC8



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

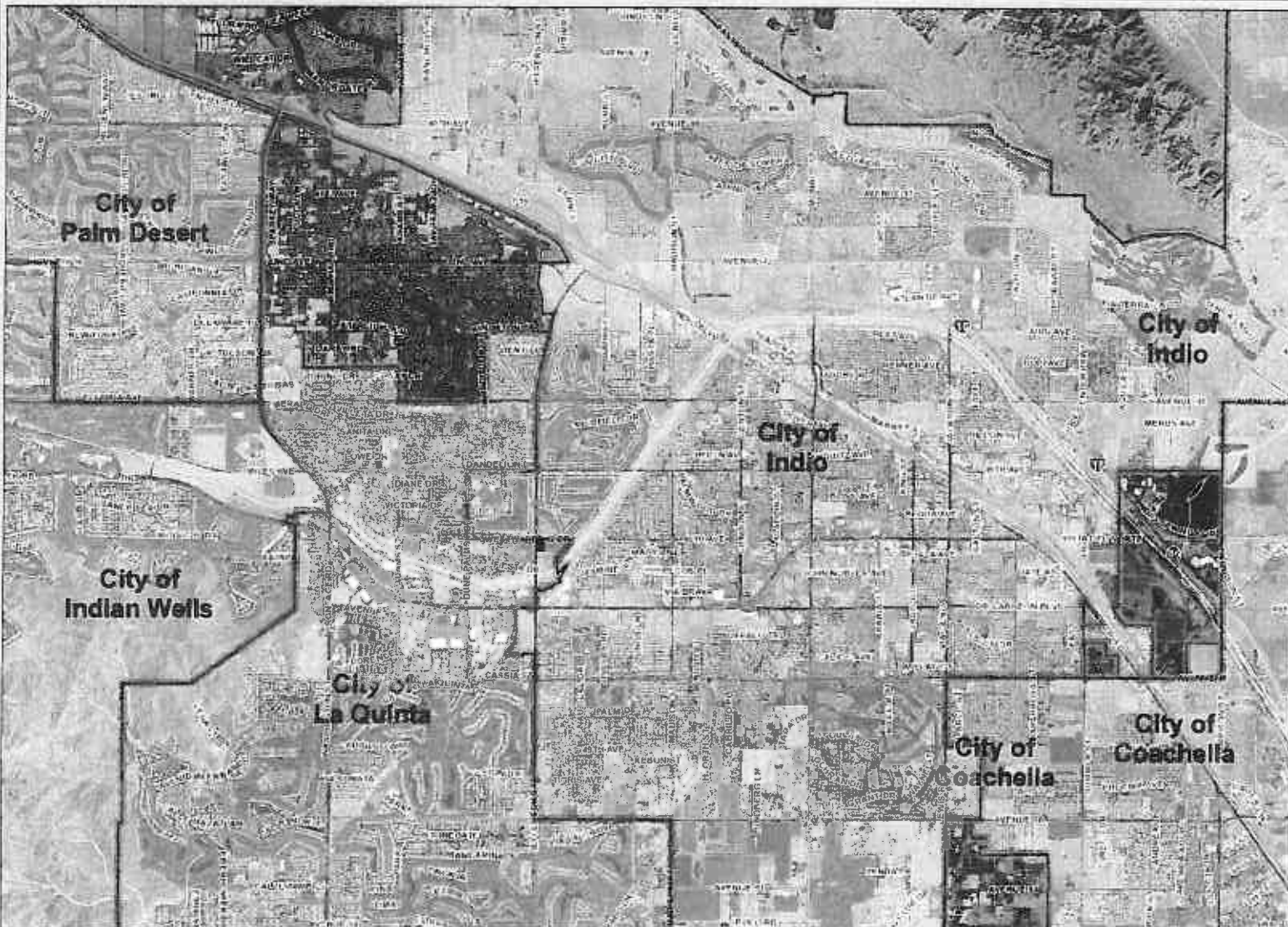
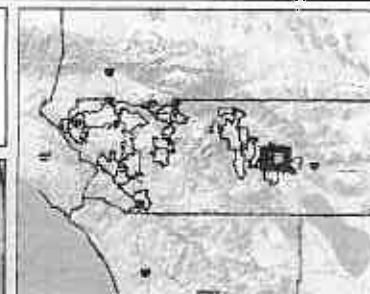


REPORT PRINTED ON... 10/19/2016 8:58:41 AM

© Riverside County RCIT GIS

Notes

My Map



Legend

- City Boundaries
- Cities
- highways_large**
- HWY
- INTERCHANGE
- INTERSTATE
- USHWY
- majorroads
- counties
- cities

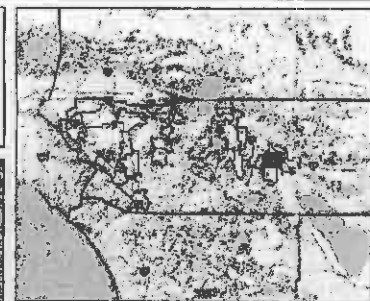
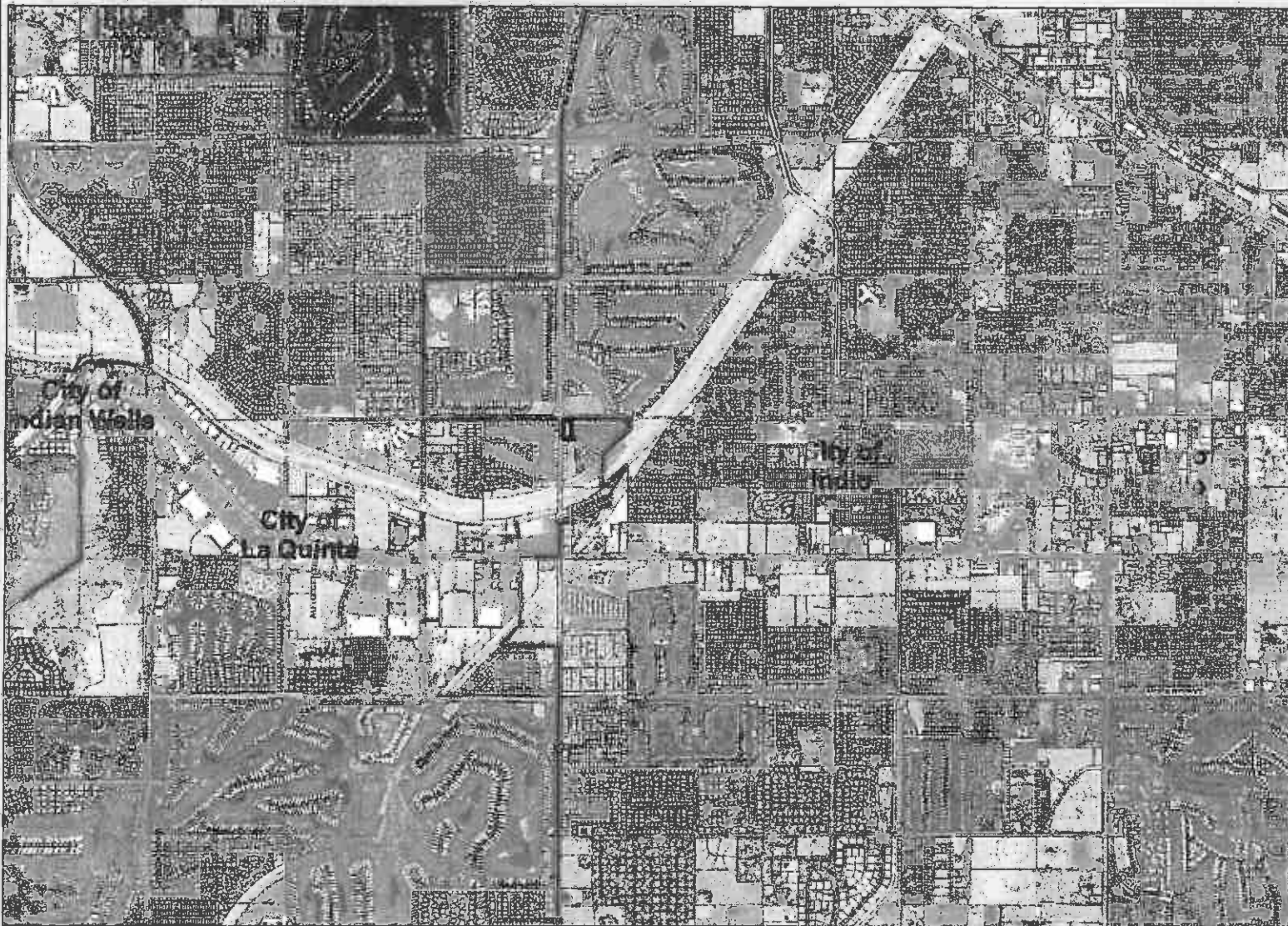
0 7,155 14,310 Feet

IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/19/2016 9:01:49 AM

Notes

My Map



Legend

- City Boundaries
- Cities**
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads**
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 3,578 7,155 Feet



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/19/2016 9:02:38 AM

© Riverside County RCIT GIS

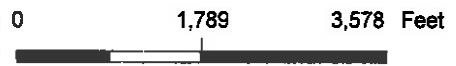
Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/19/2016 9:03:14 AM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities**
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0

894

1,789 Feet



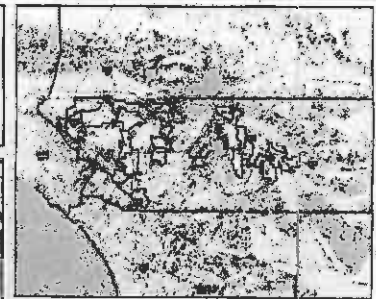
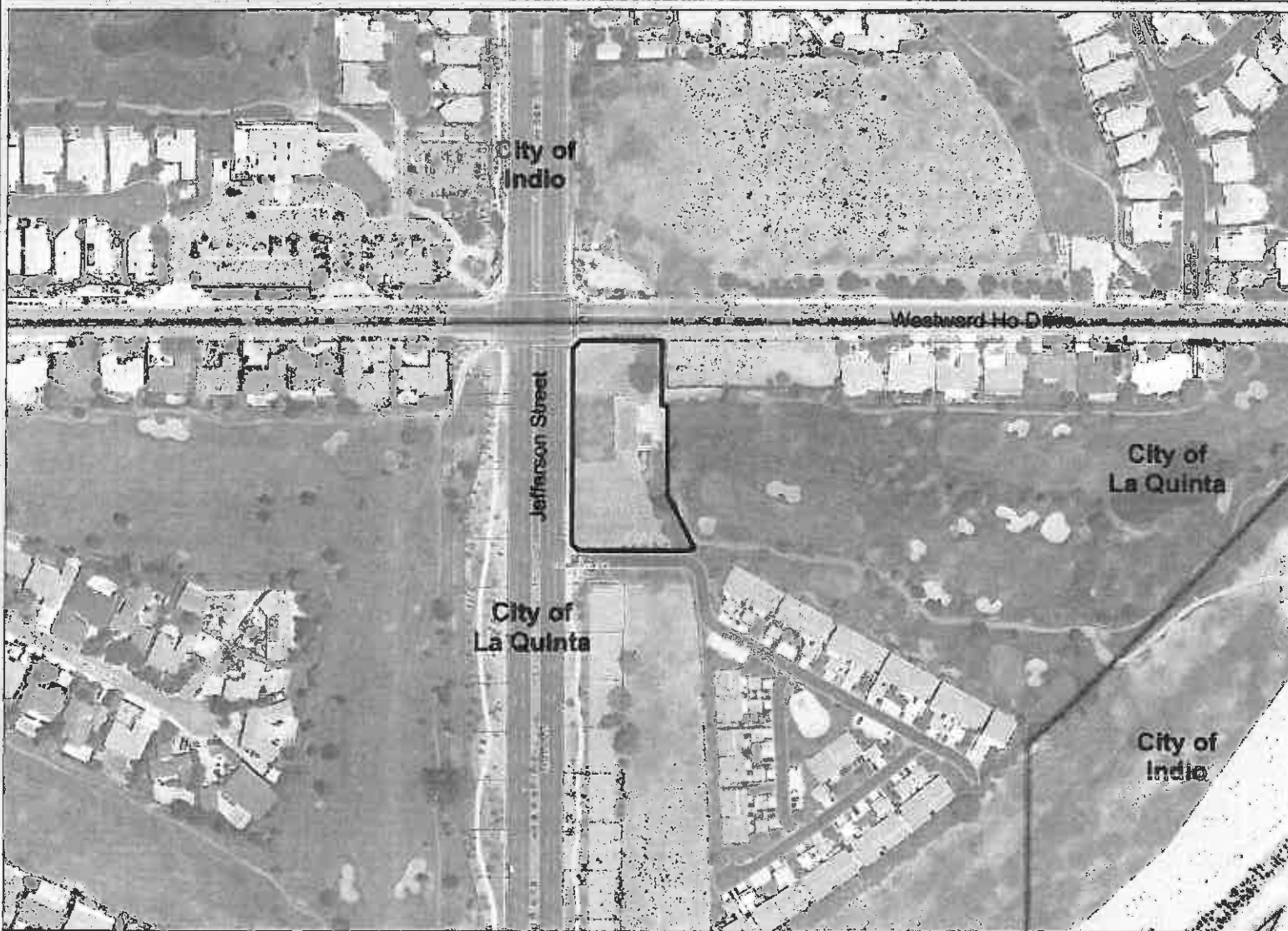
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/19/2016 9:03:57 AM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

0 311 623 Feet

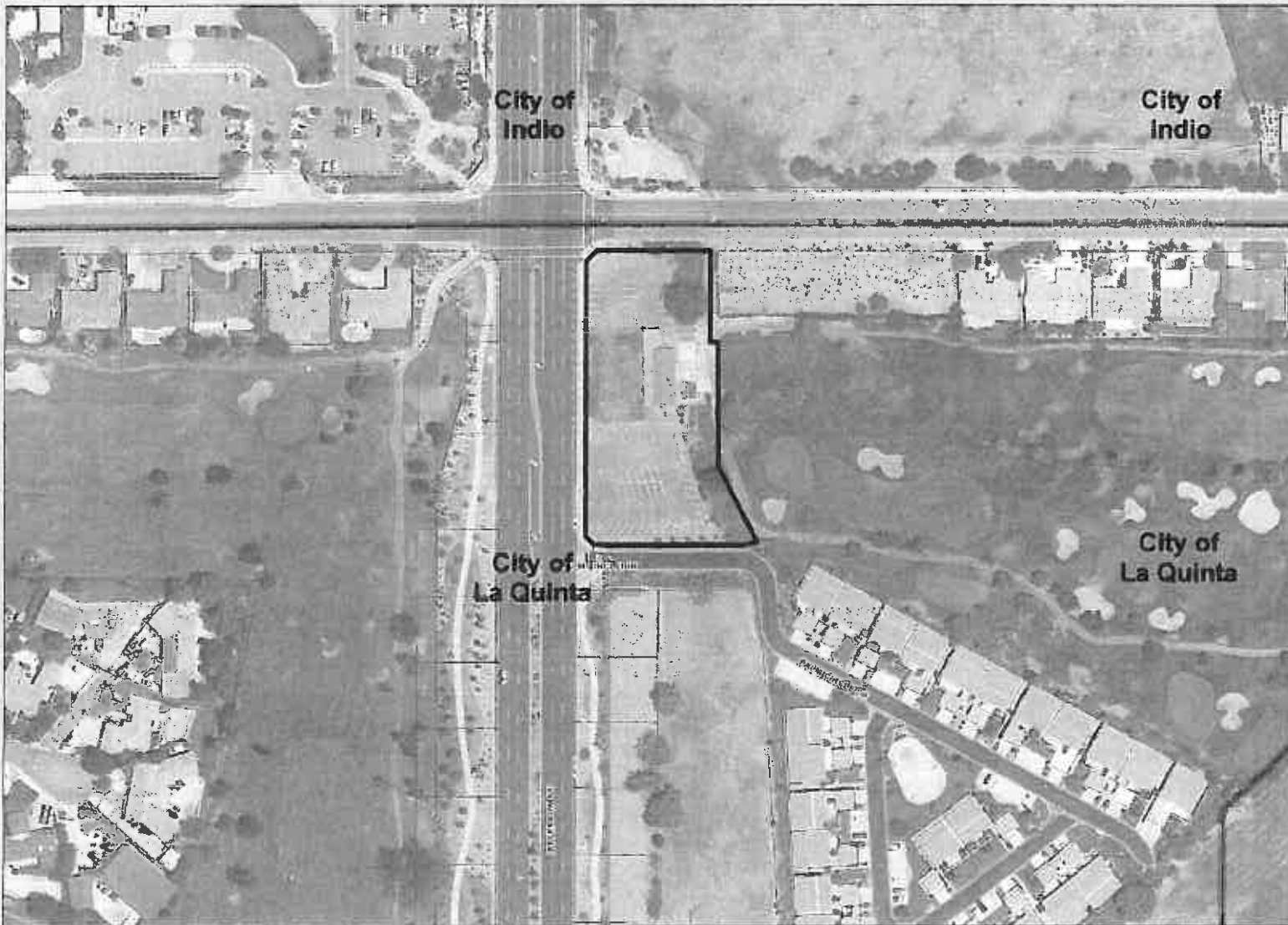
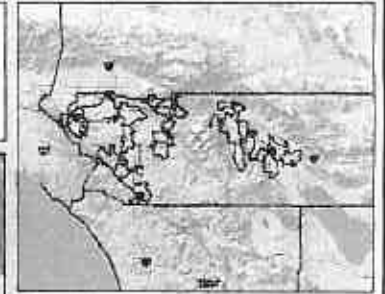


REPORT PRINTED ON... 10/19/2016 8:59:15 AM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities**
- roadsanno**
- highways**
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies**
- Lakes
- Rivers



0

224

447 Feet



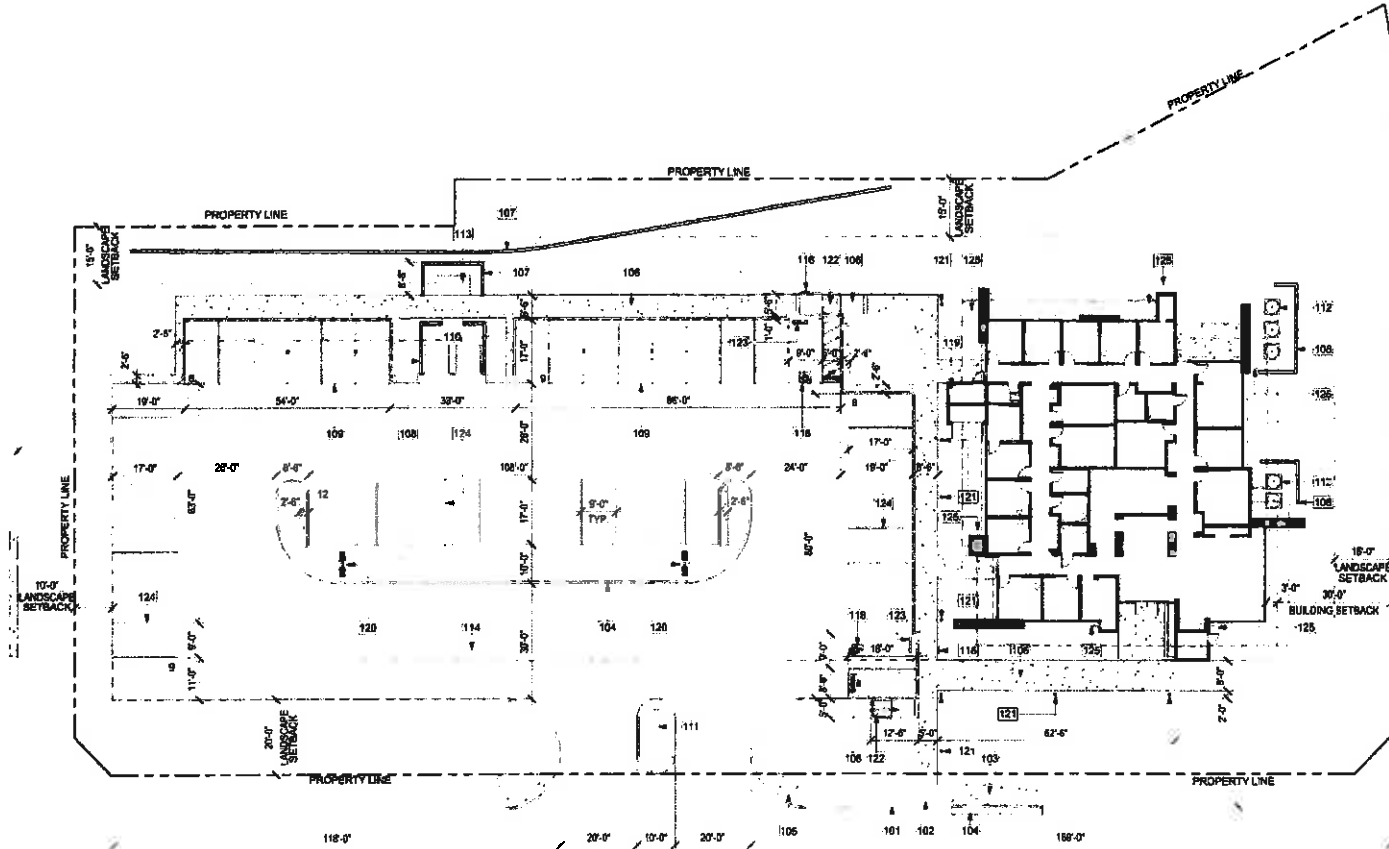
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/19/2016 9:05:58 AM

© Riverside County RCIT GIS

Notes

WESTWARDHO DRIVE



PALM CIRCLE DRIVE

JEFFERSON STREET

NO.	DESCRIPTION
101	EXISTING CONCRETE CURB AND GUTTER, SEE CIVIL
102	EXISTING CONCRETE SIDEWALK, SEE CIVIL
103	NEW CONCRETE SIDEWALK TO CONNECT TO EXISTING SIDEWALK, SEE CIVIL
104	NEW CONCRETE CURB AND GUTTER TO CONNECT TO EXISTING, SEE CIVIL
105	NEW CONCRETE DRIVEWAY APPROACH, SEE CIVIL
106	NEW ADA WALKWAYS
107	NEW RETAINING WALLS, SEE CIVIL
108	NEW SCREEN C.M./J WALL, SEE CIVIL
109	NEW CANOPY STRUCTURE
110	NEW TRASH ENCLOSURE
111	PROPOSED MOUNTMENT SIGN
112	PROPOSED AIR CONDITIONING UNIT CONCRETE PADS
113	PROPOSED ELECTRICAL TRANSFORMER
114	AC PAVEMENT, SEE CIVIL
115	NEW ADA SIGN
116	ADA PARKING
118	FIRE RISER, SEE CIVIL
120	SITE LIGHTING FIXTURE, SEE ELECTRICAL
121	LIGHT BOLLARDS, SEE ELECTRICAL
122	TRUNCATED DOME
123	WHEEL STOP
124	NEW 4" WHITE PARKING STRIPE
125	12'X30' CONCRETE SPLASH BLOCK

FARMS DATA
 MEDICAL OFFICE
 5,544 S.F. @ 1/200 = 28 REQUIRED
 44 PROVIDED

ADA ACCESSIBLE PARKING REQUIRED
 1 ADA PARKING SPACES PROVIDED
 1 ADA VAN PARKING SPACE PROVIDED

CONTOUR DERMATOLOGY
 46080 JEFFERSON STREET, LA QUINTA, CA. 92253



PREST · VUKSIC
 ARCHITECTS
 44-530 SAN PABLO AVE. STE 200 PALM DESERT, CA
 T: 760.779.5393 F: 760.779.5395

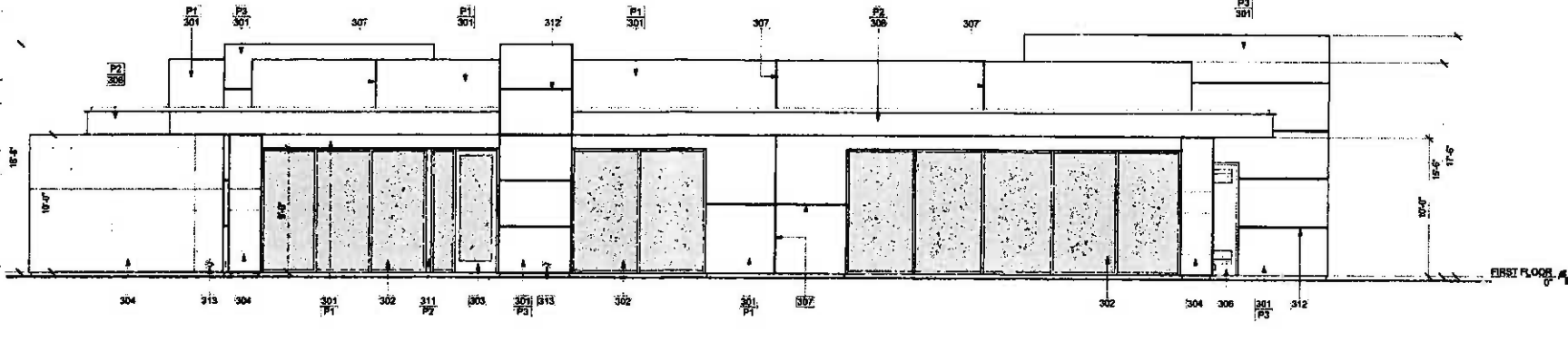
SITE PLAN

Sheet: A002

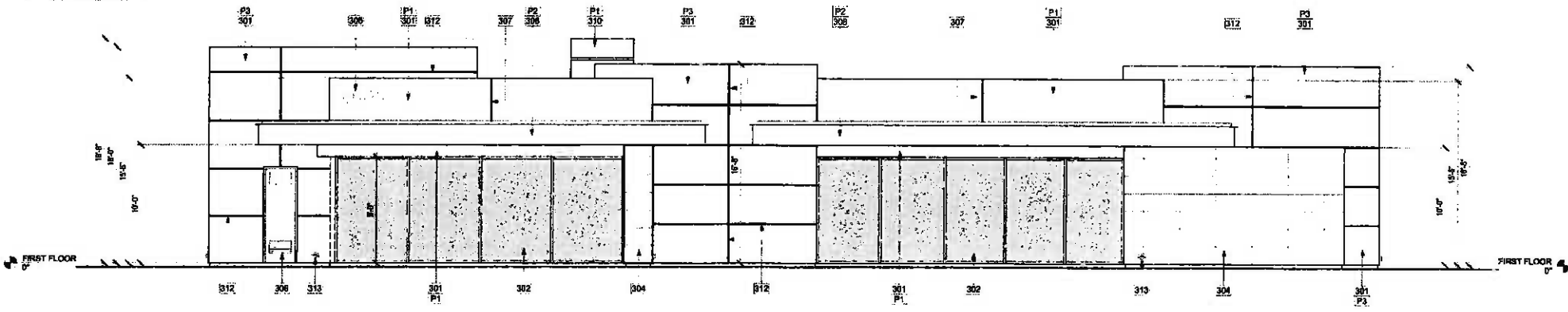
JULY 22, 2019

- KEYNOTES**
- | NO. | DESCRIPTION |
|-----|---|
| 301 | 7/8" (3 COAT) ACRYLIC STUCCO
1/2" BAND SMOOTH |
| 302 | STOREFRONT SYSTEM FRAME
COLOR: NATURAL ANODIZED
ALUMINUM |
| 303 | GLASS STOREFRONT DOOR,
FRAME COLOR: NATURAL
ANODIZED ALUMINUM |
| 304 | STONE VENEER, STONE
REVOLUTION INC. FOSSIL RUSTIC |
| 305 | ADDRESS SIGNAGE |
| 306 | HOLLOW METAL DOOR WITH
LOUVERS, PAINTED TO MATCH
ADJACENT SURFACE |
| 307 | #16 S.M. CONTROL JOINT, WRAP
AROUND CORNERS |
| 308 | W/2X44 STEEL FASCIA,
ARCHITECTURAL GRADE STEEL,
PRIME & PAINT, COLOR TO
MATCH CLEAR ANODIZED
STOREFRONT WINDOW FRAMES.
MODERN MASTERS: ME150
SILVER, 1-800-942-3188 |
| 310 | 18GA PAINT-LDC
CLADDED STEEL COLUMN TO
MATCH STOREFRONT SYSTEM,
SEE STRUCTURAL |
| 311 | FRY REGLET PCS 75-76 (WRAP
AROUND CORNERS) |
| 312 | OVERFLOW DRAIN |

- PAINT LEGEND**
- | NO. | DESCRIPTION |
|-----|--|
| P1 | MFR: DUNN EDWARDS, COLOR:
DEWASH COOL, DECEMBER |
| P2 | MFR: MODERN MASTERS, COLOR:
ME150 SILVER |
| P3 | MFR: DUNN EDWARDS, COLOR:
DEB242 WELLS GRAY |

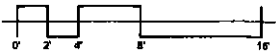


3 EAST ELEVATION
1/4" = 1'-0"



4 SOUTH ELEVATION
1/4" = 1'-0"

CONTOUR DERMATOLOGY
46080 JEFFERSON STREET, LA QUINTA, CA. 92253



PREST · VUKSIC
ARCHITECTS
44-830 SAN PABLO AVE. STE 200 PALM DESERT, CA
T: 760.779.5393 F: 760.779.5389

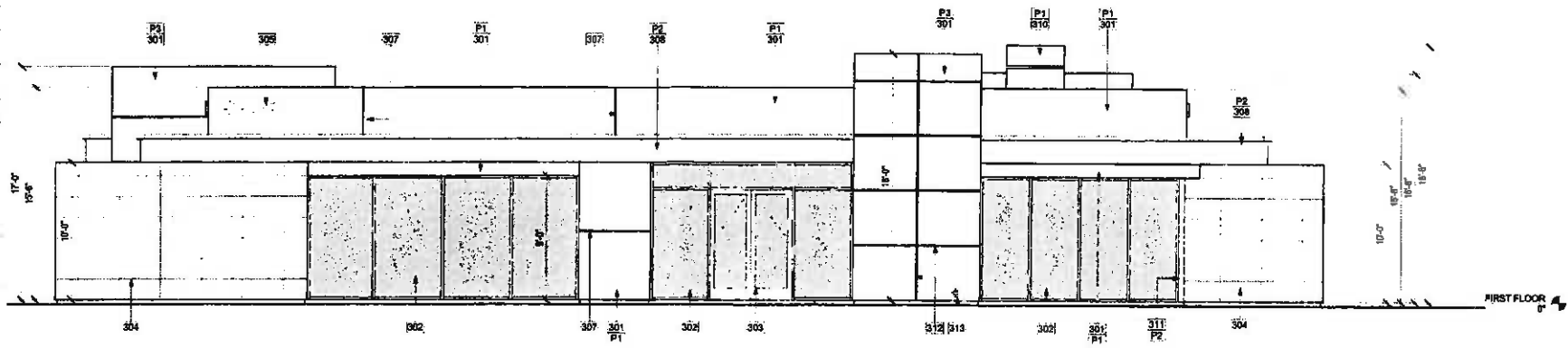
**EXTERIOR
ELEVATIONS**

Sheet: **A006**

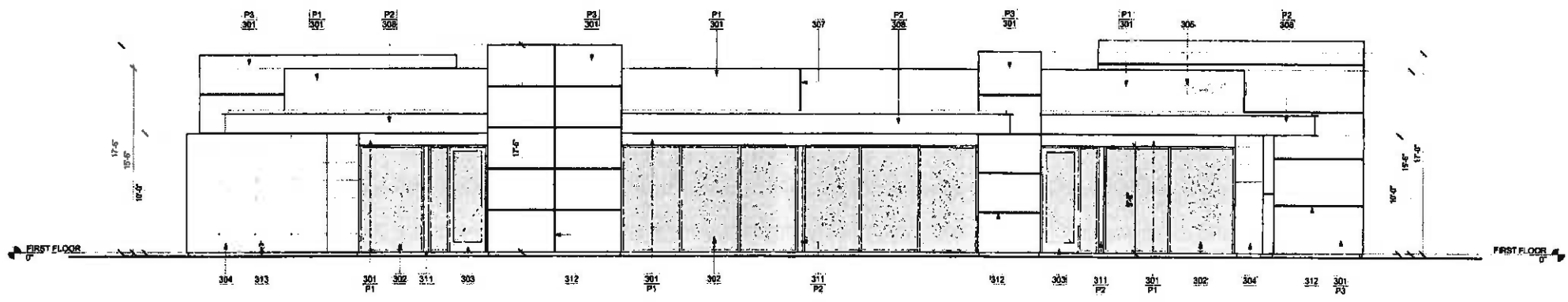
JULY 22, 2016

- KEYNOTES**
- | NO. | DESCRIPTION |
|-----|--|
| 301 | 7/8" D COATING/TYPIC STUCCO
5/32 SAND BROOM |
| 302 | STOREFRONT SYSTEM, FRAME
COLOR: NATURAL ANODIZED
ALUMINUM |
| 303 | GLASS STOREFRONT DOOR,
FRAME COLOR: NATURAL
ANODIZED ALUMINUM |
| 304 | STONE VENEER, STONE
REVOLUTION MC. FOSSIL RUSTIC |
| 305 | ADDRESS SIGNAGE |
| 306 | #13 S.S. CONTROL JOINT, WRAP
AROUND CORNERS |
| 307 | #21X44 STEEL FASCIA,
ARCHITECTURAL GRADE STEEL,
PRIME & PAINT COLOR TO
MATCH (CLEAR ANODIZED) |
| 308 | STOREFRONT WINDOW FRAMES
MODERN MASTERS: ME150
SILVER, 1-800-942-3100 |
| 310 | #824 PAINT-LOC |
| 311 | CLADDED STEEL COLUMN TO
MATCH STOREFRONT SYSTEM,
SEE STRUCTURAL |
| 312 | FRY REGLET PC8 75-75 (WRAP
AROUND CORNERS) |
| 313 | OVERFLOW DRAIN |

- PAINT LEGEND**
- | NO. | DESCRIPTION |
|-----|---|
| P1 | MFR: DUNN EDWARDS, COLOR:
DENVER COOL, DECHEMBER |
| P2 | MFR: MODERN MASTERS, COLOR:
ME150 SILVER |
| P3 | MFR: DUNN EDWARDS, COLOR:
DEB242 WELLS GRAY |

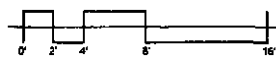


1 WEST ELEVATION
1/4" = 1'-0"



2 NORTH ELEVATION
1/4" = 1'-0"

CONTOUR DERMATOLOGY
46080 JEFFERSON STREET, LA QUINTA, CA. 92253

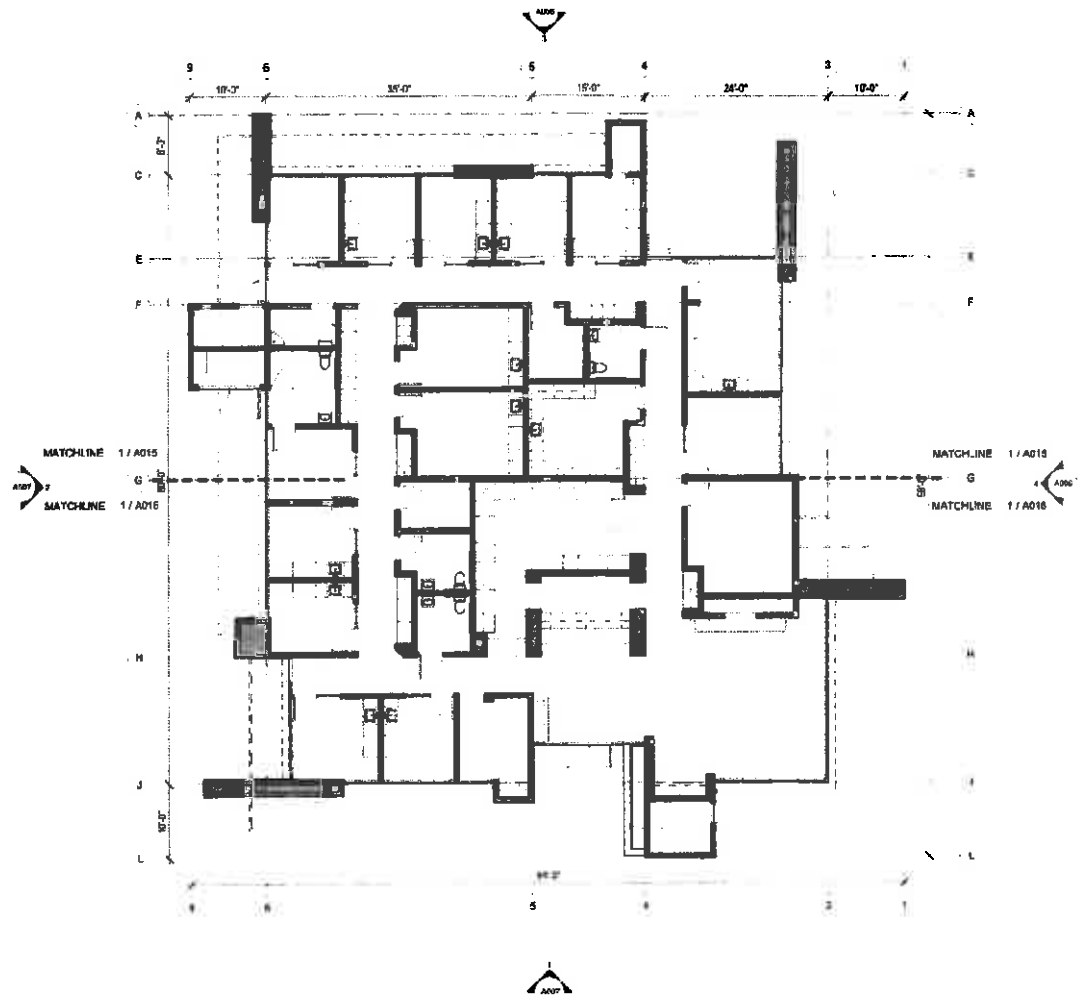


PREST · VUKSIC
ARCHITECTS
44-530 SAN PABLO AVE., STE 200 PALM DESERT, CA
T: 760.778.5393 F: 760.778.5395

EXTERIOR
ELEVATIONS

Sheet: **A007**

JULY 22 2018



CONTOUR DERMATOLOGY
 46080 JEFFERSON STREET, LA QUINTA, CA. 92253



PREST · VUKSIC
ARCHITECTS
 44-930 SAN PABLO AVE. STE 200 PALM DESERT, CA
 T. 760.779.5393 F. 760.779.5395

OVERALL FLOOR PLAN

Sheet **A014**

JULY 22, 2016

PAGE BREAK



**AIRPORT LAND USE COMMISSION
RIVERSIDE COUNTY**



CHAIR
Simon Housman
Rancho Mirage

VICE CHAIRMAN
Rod Ballance
Riverside

COMMISSIONERS

Arthur Butler
Riverside

John Lyon
Riverside

Glen Holmes
Hemet

Steve Manos
Lake Elsinore

Russell Betts
Desert Hot Springs

November 4, 2016

Ms. Gaby Adame, Project Planner
City of Riverside Community and Economic Development Department/Planning Division
3900 Main Street, Third Floor
Riverside, CA 92522

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –
DIRECTOR’S DETERMINATION**

File No.: ZAP1223MA16
Related File Nos.: P16-0082 (Conditional Use Permit), P16-0083 (Design Review)
APN: 250-150-002, 250-150-013

Dear Ms. Adame:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed City of Riverside Case Nos. P16-0082 (Conditional Use Permit) and P16-0083 (Design Review), pertaining to a proposal to demolish an existing restaurant building and construct 6 new fuel pumps (with a 3,268 square foot fueling canopy area) as an addition to an existing gasoline service station and a 1,960 square foot self-serve car wash on a 0.88-acre site that also includes an existing convenience store located at 1340-1360 West Blaine Street, westerly of Iowa Avenue and northeasterly of the Interstate 215 Freeway.

The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, nonresidential intensity is not restricted.

The site’s finished floor elevation is 968 feet AMSL, and the tallest building/structure height is 31 feet, resulting in a maximum top point elevation of 999 feet AMSL. This is more than 500 feet lower than the elevation of March’s Runway 14-32 at its northerly terminus (approximately 1,535 feet above mean sea level). The site is located less than 20,000 feet from Runway 6-24 at Flabob Airport, with an elevation of 768 feet above mean sea level (AMSL). However, as the length of this runway does not exceed 3,200 feet, the relevant horizontal distance is 10,000 feet. This site is located 19,500 feet from the runway. Therefore, FAA OES review was not required for height/elevation reasons.

As ALUC Director, I hereby find the above-referenced projects **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, subject to the following conditions:

STAFF
Director
Ed Cooper
John Guerin
Paul Rull
Barbara Santos
County Administrative Center
4080 Lerron St., 14th Floor
Riverside, CA 92501
(951) 955-5132

www.rcaluc.org

AIRPORT LAND USE COMMISSION

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris centers, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The landowner shall provide the attached notice to all potential purchasers of the property and to tenants/lessees of the building(s) thereon.

If you have any questions, please contact Paul Rull, ALUC Urban Regional Planner IV, at (951) 955-6893 or John Guerin, ALUC Principal Planner, at (951) 955-0982.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Edward C. Cooper, Director

Attachments: Notice of Airport in Vicinity

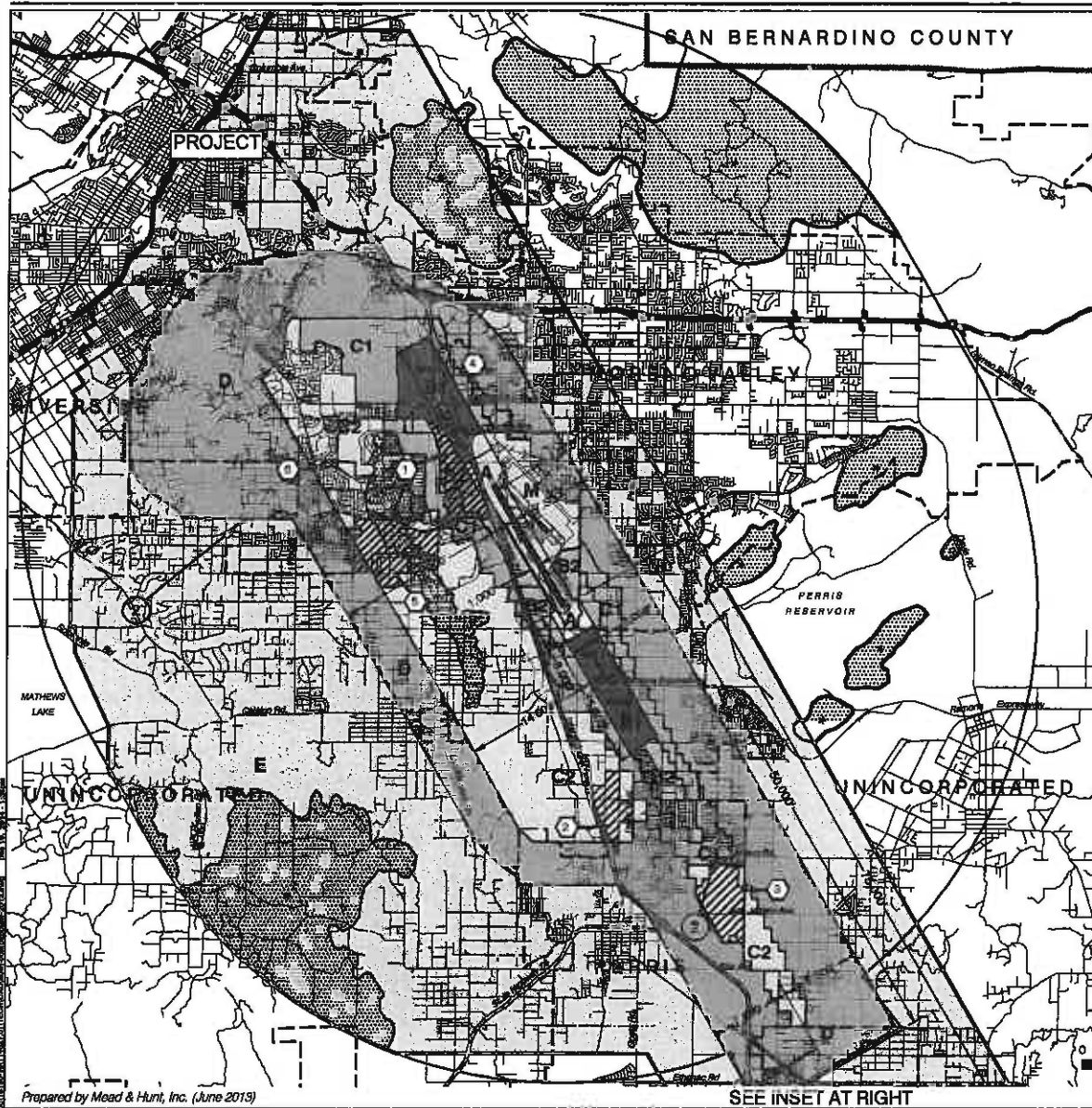
AIRPORT LAND USE COMMISSION

cc: Gurpirka Properties, LLC (applicant/property owner/payee)
Fred Cohen, CJC Design Inc. (representative)
Surinder P. Singh (property owner)
Gary Gosliga, Airport Manager, March Inland Port Airport Authority
Denise Hauser, March Air Reserve Base
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1223MA16\ZAP1223MA16.LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



LEGEND

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

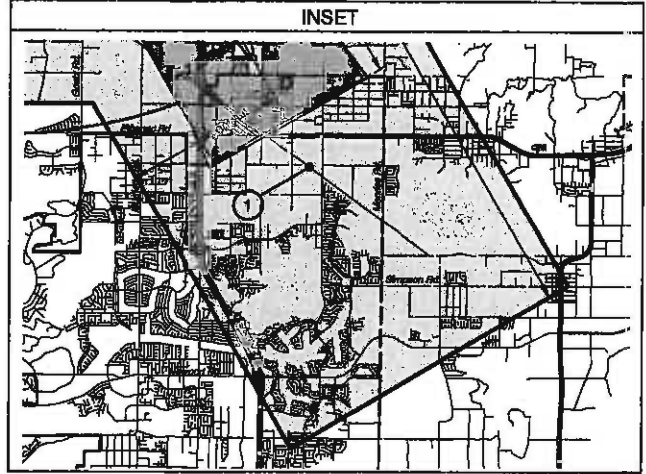
Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- ▨ Site-Specific Exceptions (existing local agency commitments to development projects)

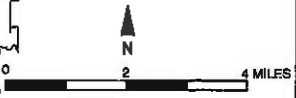
① Point at which aircraft on Runway 92 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.

② Point at which departing aircraft typically reach 3,000 feet above runway end.

① March JPA: March Business Center/Meridian
 ② Perris: Harvest Landing
 ③ Perris: Park West
 ④ Moreno Valley: Affordable Housing
 ⑤ March JPA: Ben Clark Training Center
 ⑥ Riverside: Ridge Crest Subdivision



Note:
All dimensions are measured from runway ends and centerlines.



Base map source: County of Riverside 2013

Riverside County
 Airport Land Use Commission
 March Air Reserve Base / Inland Port Airport
 Land Use Compatibility Plan
 (Adopted November 13, 2014)

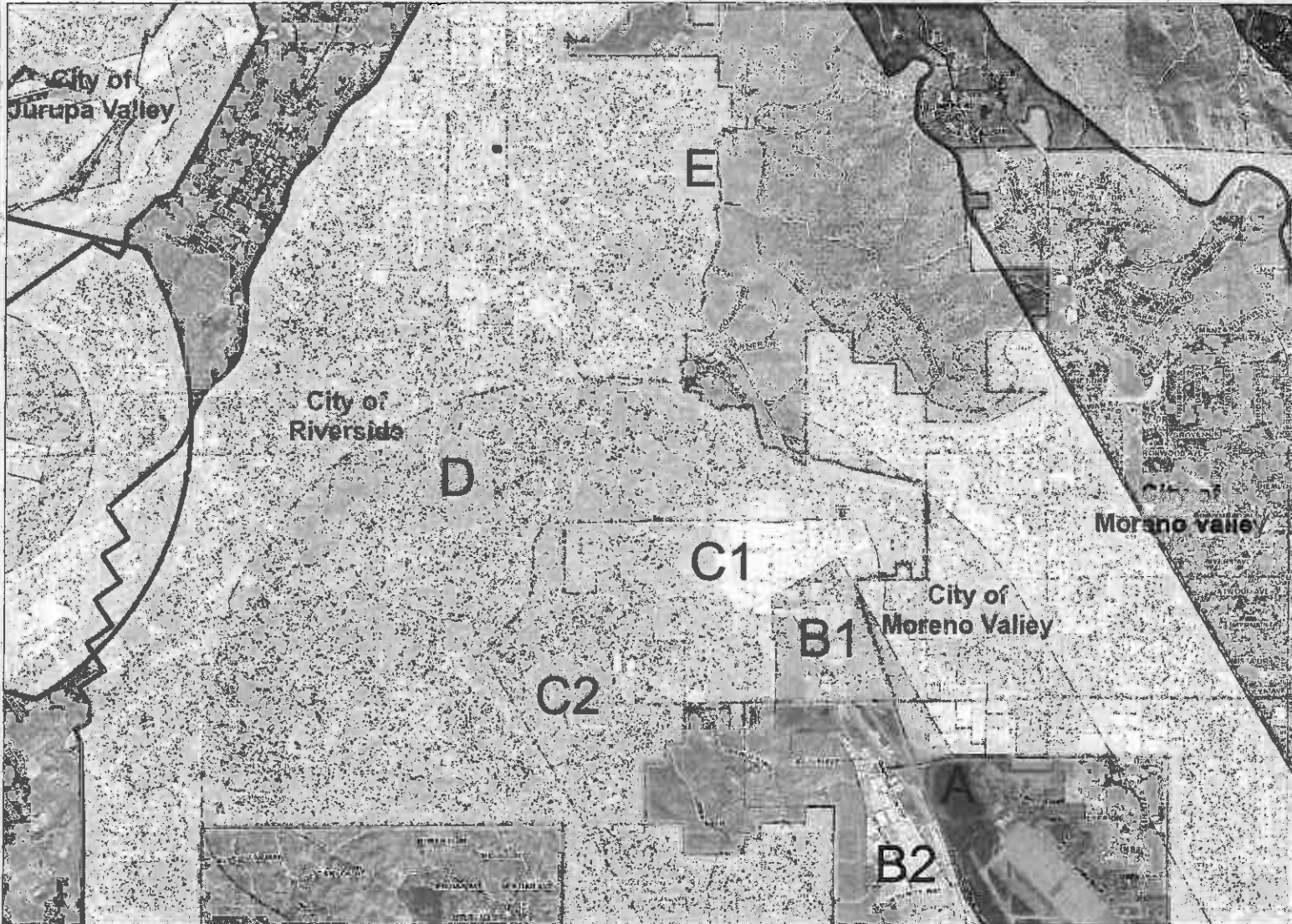
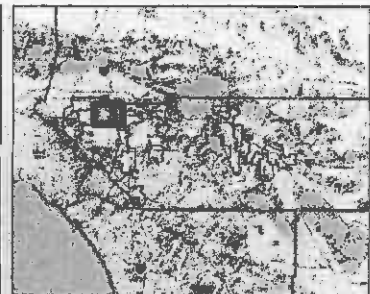
Map MA-1

Compatibility Map
 March Air Reserve Base / Inland Port Airport

Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

My Map



Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



0 8,177 16,355 Feet



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/25/2016 11:46:13 AM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



0 107 215 Feet



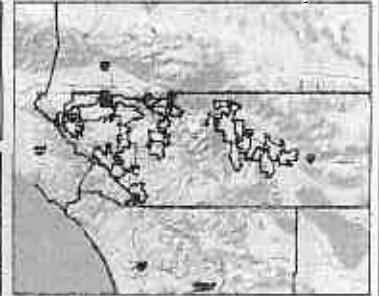
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/25/2016 11:43:24 AM
















© Riverside County RCIT GIS

Notes

My Map



Legend

-  City Boundaries
- Cities**
- roads**
- highways**
 -  HWY
 -  INTERCHANGE
 -  INTERSTATE
 -  OFFRAMP
 -  ONRAMP
 -  USHWY
- roads**
 -  Major Roads
 -  Arterial
 -  Collector
 -  Residential
-  counties
-  cities
- hydrography**
- waterbodies**
 -  Lakes
 -  Rivers



0 2,044 4,089 Feet



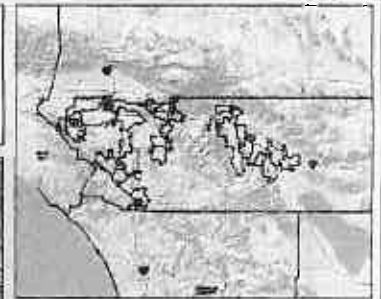
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/25/2016 11:47:48 AM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 1,022 2,044 Feet



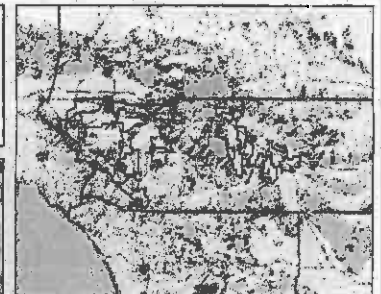
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/25/2016 11:48:13 AM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities**
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies**
- Lakes
- Rivers



0 511 1,022 Feet



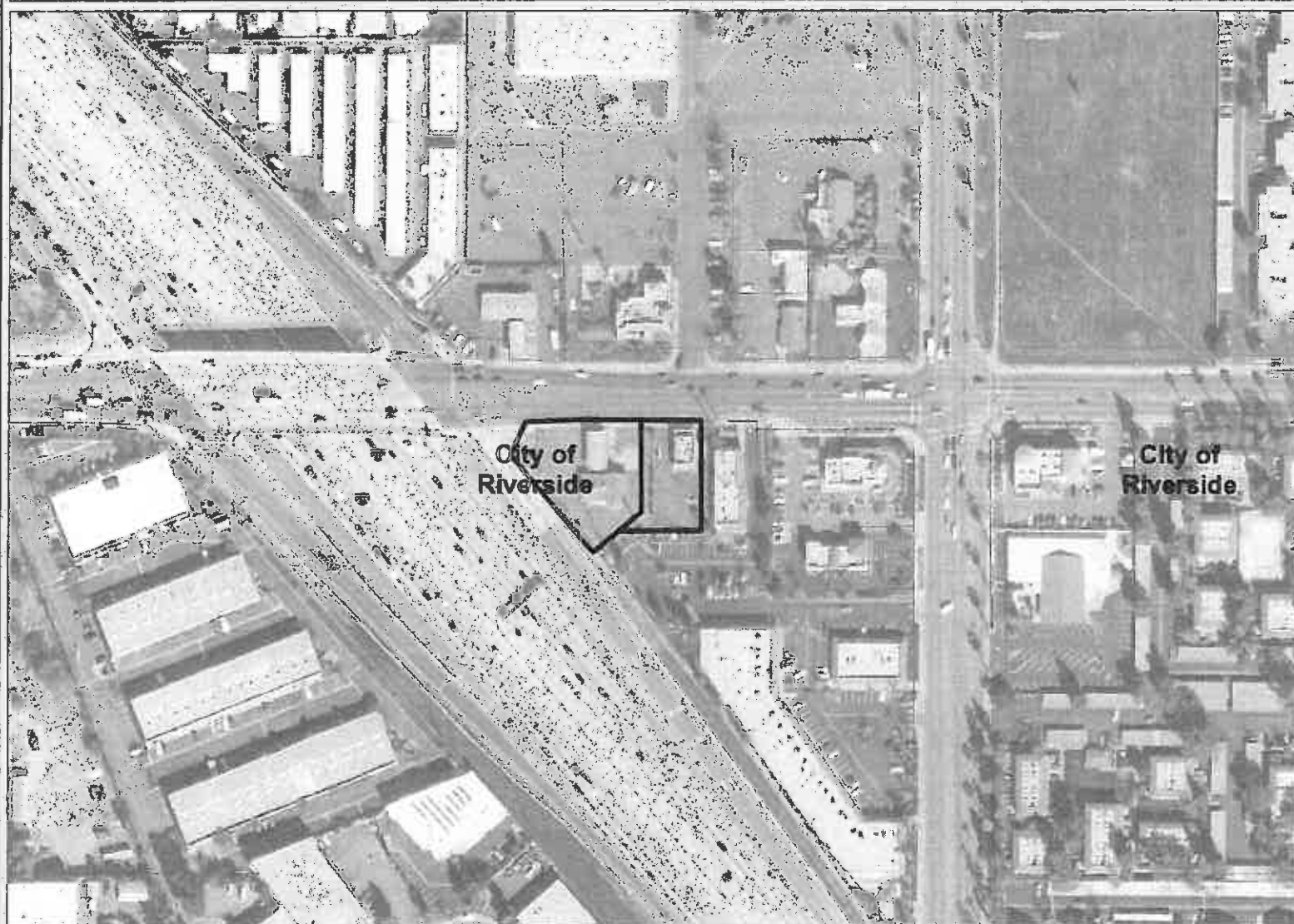
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/25/2016 11:48:50 AM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities**
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies**
- Lakes
- Rivers



0 256 511 Feet



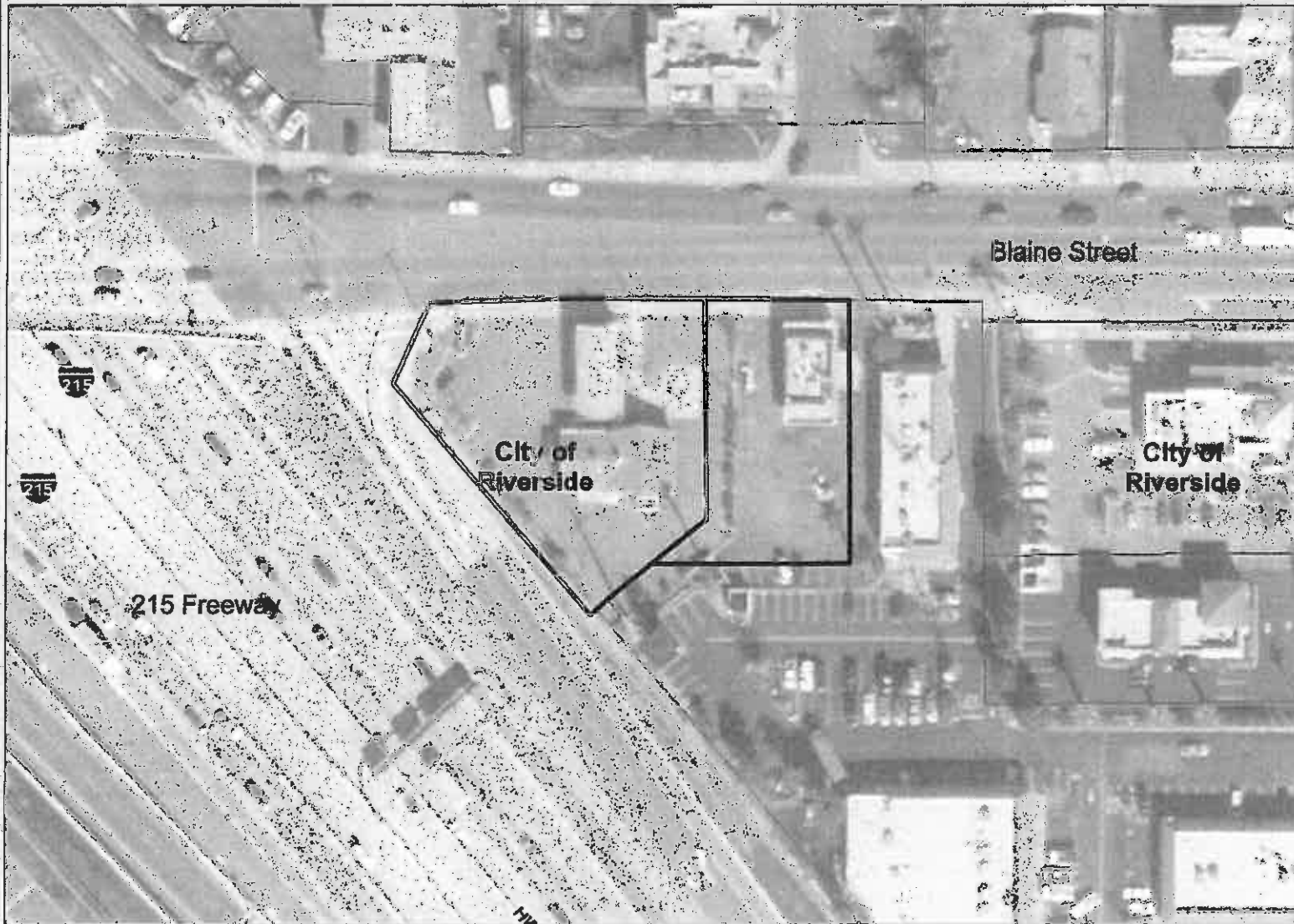
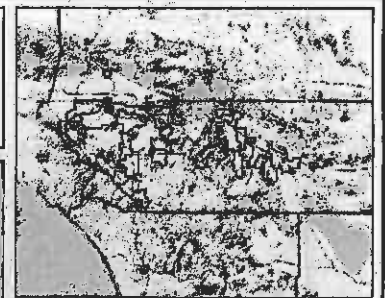
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/25/2016 11:49:17 AM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 107 215 Feet



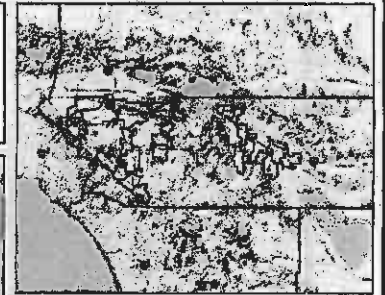
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/25/2016 11:43:57 AM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities
- roadsanno
- highways
 - HWY
 - INTERCHANGE
 - INTERSTATE
 - OFFRAMP
 - ONRAMP
 - USHWY
- counties
- cities
- hydrographylines
- waterbodies
 - Lakes
 - Rivers



0 64 128 Feet

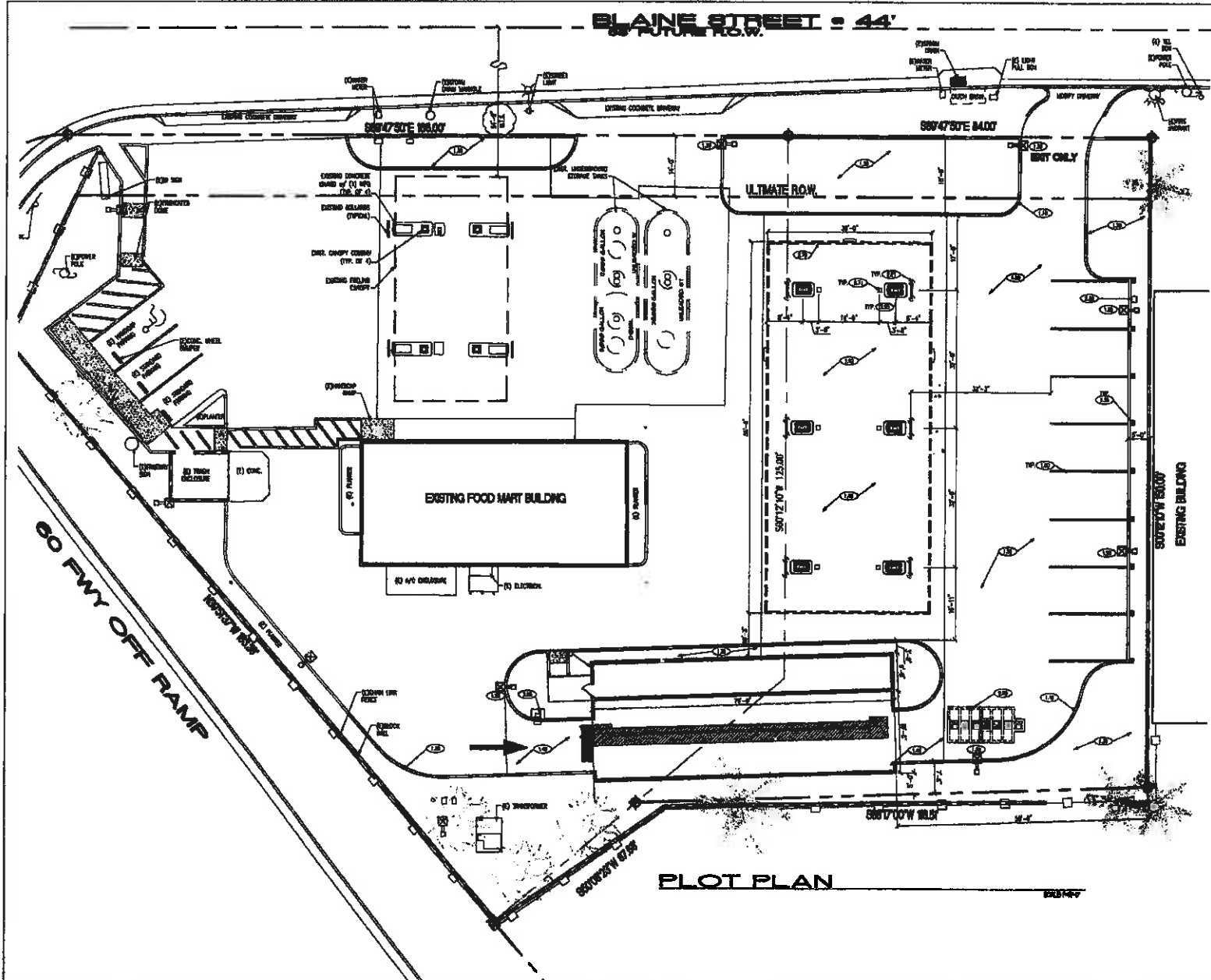


IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

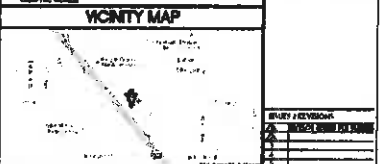
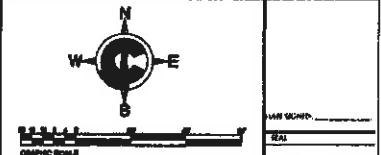
REPORT PRINTED ON... 10/25/2016 11:49:45 AM

© Riverside County RCIT GIS

Notes



BLAINE STREET = 44'
 30' FUTURE R.O.W.



- CONSTRUCTION NOTES**
- 1.10 NEW 4" CONCRETE CURB
 - 1.20 NEW AREA LIGHT
 - 1.30 NEW LANDSCAPE WITH AUTOMATIC IRRIGATION
 - 1.40 NEW 4" CONCRETE DRIVE SLAB WITH 6" SAND IN 1" GC. DR AS SPECIFIED IN SUELS REPORT IF AVAILABLE.
 - 1.50 NEW 4" CURB AT A.K. SEWER MAIN OR AS SHOWN IN SUELS REPORT IF AVAILABLE.
 - 1.60 NEW 8'x11' STANDARD PARKING SPACE
 - 2.20 NEW 4" CONCRETE FLEED STEEL CURB POSTS
 - 2.21 NEW 8-SHAPED STEEL CURB POSTS
 - 2.30 NEW 4" SHAPED STEEL CURB POSTS
 - 2.40 NEW 4" SHAPED CURB
 - 2.50 NEW 4" SHAPED CURB
 - 2.60 NEW CONCRETE CURB W/ (1) 1/2" MAX PROJECT DEPTH (IF 4" W/ 4" MAX. HEIGHT)
 - 2.70 NEW FLEEDING CURB
 - 2.71 NEW FLEEDING CURB COLLARS
 - 2.80 NEW 4" SHAPED 4"X4" INTERCEPTOR
 - 2.85 NEW 4" SHAPED 4"X4" INTERCEPTOR

SITE DATA INFORMATION

TYPE	SIZE	BOUND	TERRAIN
LANDSCAPE	17'x17'	1	1
REGULAR	8'x11'	3	3
REGULAR	17'x17'	0	0
TOTAL		10	11

LANDSCAPE AREA	BL. FT.	ACR.	ACR.
LANDSCAPE AREA	29,042	0.66	0.66
CONCRETE	1,768	-	4.00
CONCRETE	4,580	-	10.28
CONCRETE	1,800	-	4.10
LANDSCAPE AREA	7,478	-	16.80

PARKING CALCULATION/ANALYSIS

CONCRETE SIDE AREA = 1,768

PARKING REQUIRED = 176 / 1000 x 4 = 0.7 SPACES

2nd STORY OFFICE/STORAGE AREA = 2904

PARKING REQUIRED = 290 / 1000 x 1 = 0.29 SPACES

OFF WORK AREA = 1750

PARKING REQUIRED = 1750 / 1000 = 1.75 SPACES

TOTAL PARKING REQUIRED = 2.74 SPACES

TOTAL PARKING PROVIDED = 3 SPACES

BLAINE AVENUE
 1300 W. BLAINE STREET
 600' W. W.

PROJECT NAME AND ADDRESS

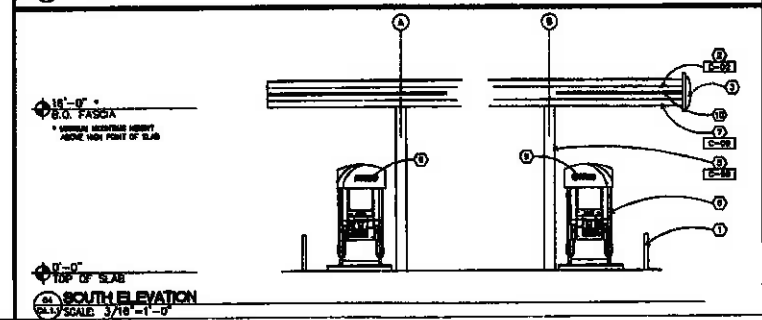
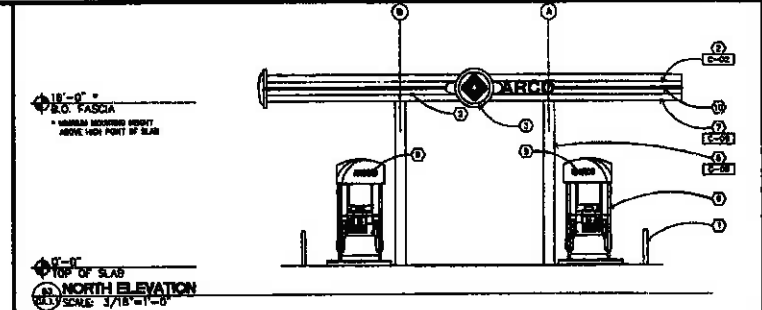
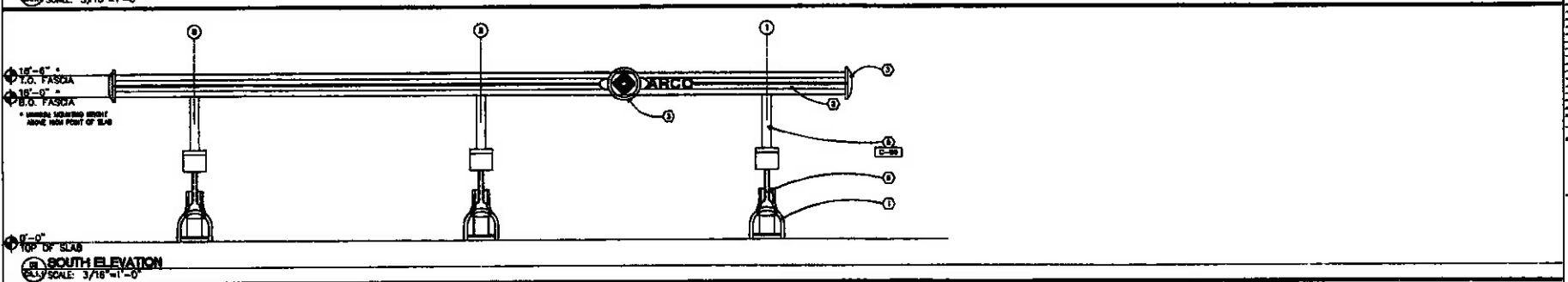
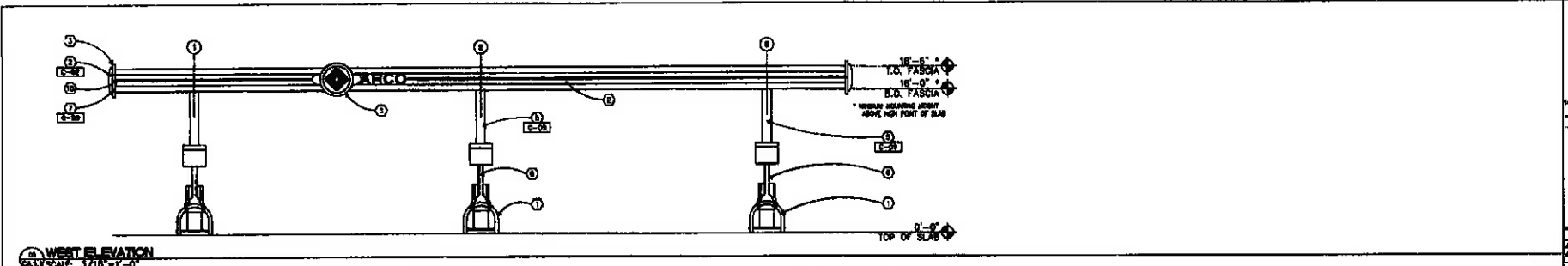
CJC Design, Inc.
 1001 West Street, Suite 202
 Los Angeles, CA 90017
 (213) 481-1111



PLOT PLAN

PROJECT NO: 15048

C11



KEYED NOTES

- 1) ANCHOR BOLTS
- 2) WELD BEGAL APPLIED TO JOINT PANEL
- 3) ALLOWED ONE SIDE BY OTHER FIELD VERIFY LOCATION
- 4) STEEL BEAM, REFER TO STRUCTURAL
- 5) STEEL COLUMN, REFER TO STRUCTURAL
- 6) PANEL SUPPORT AND CURB BY OTHERS
- 7) ALUMINUM COMPOSITE MATERIAL PANELS BY OTHERS
- 8) WET SIDE
- 9) 2" x 4" DIMENSION WITH COMPOSITE ON WET FACE OF COLUMN (TYP.)
- 10) USE "LARGE LIGHT" SETUP TO MATCH

EXTERIOR PAINTS

- C-21 COLOR: ARCO BLUE
PVC 2000 - SATIN FINISH
- C-22 COLOR: NEAR
SILVER PALE 1013 - SEMI-GLOSS
- C-23 COLOR: ARCO LIGHT BLUE
PVC 2000 - SATIN FINISH

DATE: 10/14/11
 1/11
 2/11
 3/11
 4/11
 5/11
 6/11
 7/11
 8/11
 9/11
 10/11
 11/11
 12/11

PROJECT: CANOPY FOR OIL FIELD
 CLIENT: ARCO
 10000 W. 10TH STREET
 TORRANCE, CA 90503
 PHONE: (562) 409-1111
 FAX: (562) 409-1112
 WWW.ARCO.COM

GJC Design, Inc.
 10000 W. 10TH STREET, TORRANCE, CA 90503
 (562) 409-1111
 (562) 409-1112
 WWW.GJCD.COM

ARCO
 THE WORLD'S LEADING
 MOTOR OIL COMPANY

CANOPY ELEVATIONS

PROJECT NO: 15043

CA.11

PAGE BREAK





AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

November 7, 2016

CHAIR
Simon Housman
Rancho Mirage

VICE CHAIRMAN
Rod Ballance
Riverside

COMMISSIONERS

Arthur Butler
Riverside

John Lyon
Riverside

Glen Holmes
Hemet

Steve Manos
Lake Elsinore

Russell Betts
Desert Hot Springs

STAFF

Director
Ed Cooper

John Guerin
Paul Rull
Barbara Santos

County Administrative Center
4080 Larron St., 14th Floor
Riverside, CA 92501
(951) 955-6132

www.rcaluc.org

Ms. Patty Nevins, Senior Planner
City of Banning Community Development Department/Planning Division
P. O. Box 998
99 E. Ramsey Street
Banning CA 92220

RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW – DIRECTOR'S DETERMINATION

File No.: ZAP1022BA16
Related File No.: CUP 16-8002 (Conditional Use Permit), DR 16-7003 (Design Review)
APN: 543-090-019

Dear Ms. Nevins:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed City of Banning Case No. CUP 16-8002 (Conditional Use Permit) and DR 16-7003 (Design Review), a proposal to construct a 10,500 square foot building 24 feet in height for indoor storage of recreational vehicles (RVs) and contractors' equipment and to operate an equipment rental and RV storage yard on a 4.5 acre parcel, located northerly of Charles Street, southerly of Barbour Street, easterly of Driftwood Circle, and westerly of Hathaway Street in the City of Banning.

The site is located within Airport Compatibility Zone E of the Banning Municipal Airport Influence Area (AIA). Compatibility Zone E does not restrict nonresidential intensity.

The elevation of Runway 8-26 at Banning Municipal Airport is approximately 2,219 feet above mean sea level (2219 feet AMSL). At a distance of approximately 1,969 feet from the runway to the above-referenced parcel, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 2,239 feet AMSL. The existing site elevation is approximately 2,205 feet AMSL. The proposed maximum height of the building is 24 feet, for an approximate total maximum elevation of 2,229 feet AMSL. Therefore, FAA Obstruction Evaluation Service review for height/elevation reasons is not required.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2004 Banning Municipal Airport Land Use Compatibility Plan, subject to the following conditions:

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent

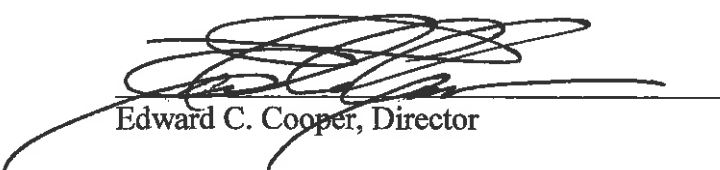
AIRPORT LAND USE COMMISSION

either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.

2. The following uses shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, artificial marshes, wastewater management facilities, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached notice shall be provided to all potential purchasers of the property and to tenants of the proposed building.
4. Any new aboveground detention basins on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be used in project landscaping.

If you have any questions, please contact Paul Rull, ALUC Urban Regional Planner IV, at (951) 955-6893 or John Guerin, ALUC Principal Planner, at (951) 955-0982.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Edward C. Cooper, Director

AIRPORT LAND USE COMMISSION

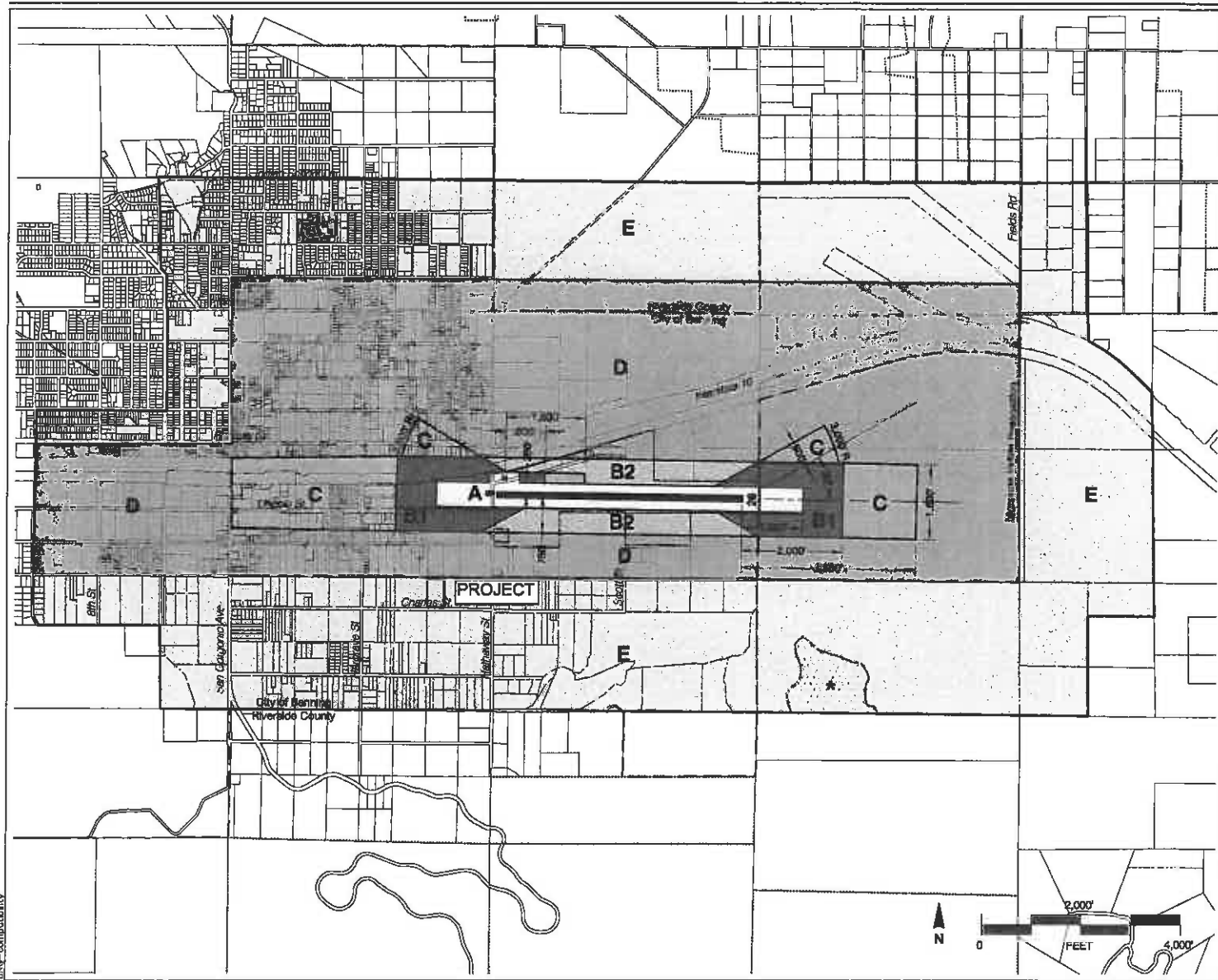
Attachments: Notice of Airport in Vicinity

cc: Andy and Rebekah Anderson (applicant/representative/property owner/payee)
Carl Szoyka, Airport Manager, City of Banning
ALUC Case File

Y:\AIRPORT CASE FILES\Banning\ZAP1022BA16\ZAP1022BA16.LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Legend

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E
- Height Review Overlay Zone

Boundary Lines

- Airport Property Line
- City Limits
- Morongo Indian Reservation

Note

Dimensions measured from runway ends and centerlines.
See Chapter 2, Table 2A for compatibility criteria associated with this map.

Riverside County
Airport Land Use Commission
**Riverside County
Airport Land Use Compatibility Plan
Policy Document**
(Adopted October 2004)

Map BN-1

Compatibility Map
Banning Municipal Airport

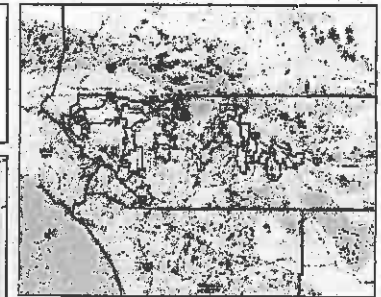
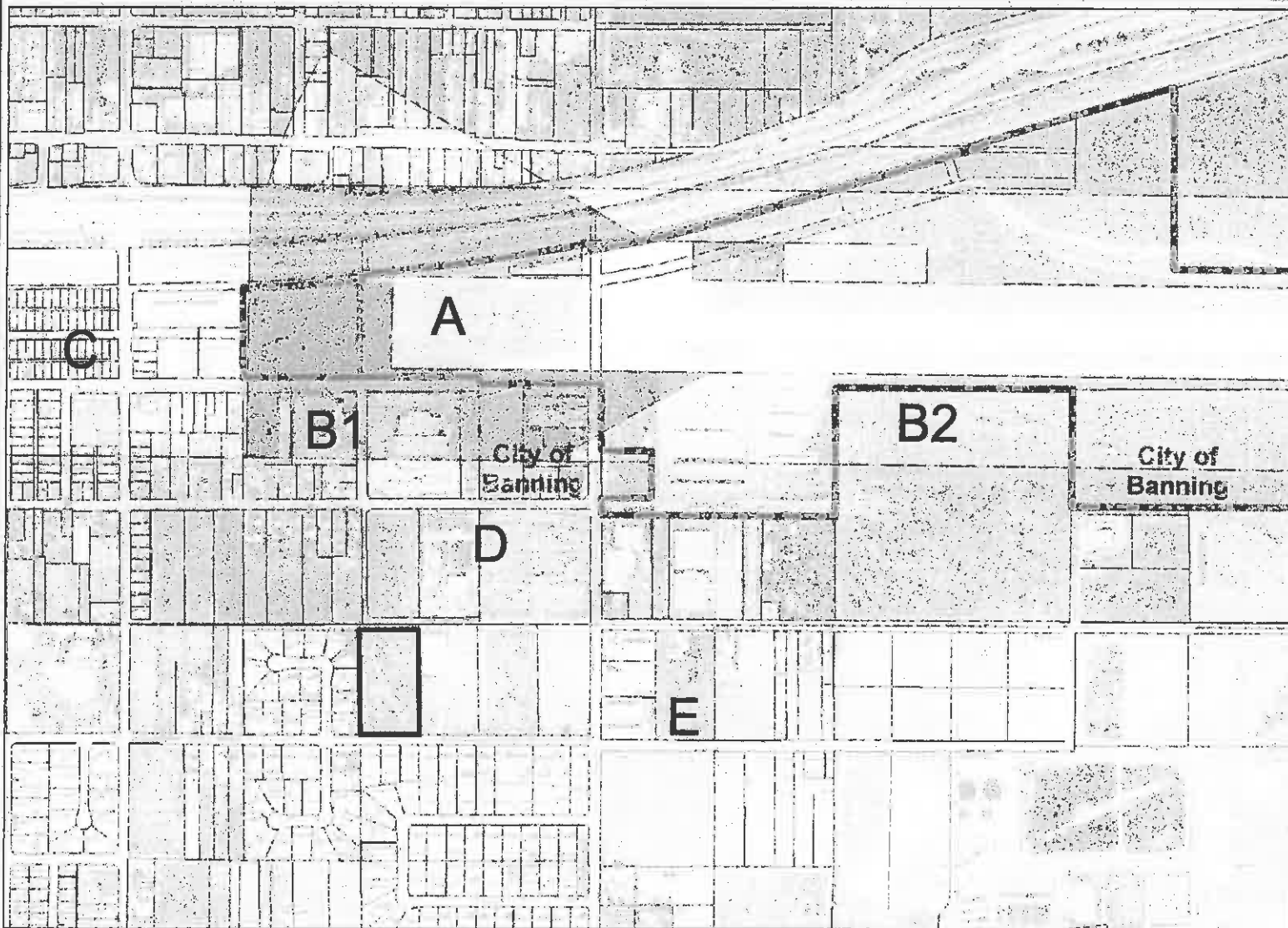
BNC-compatibility

Charles St
Banning, CA

Legend
Charles St



My Map



Legend

- Display Parcels
- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



0 1,033 2,066 Feet



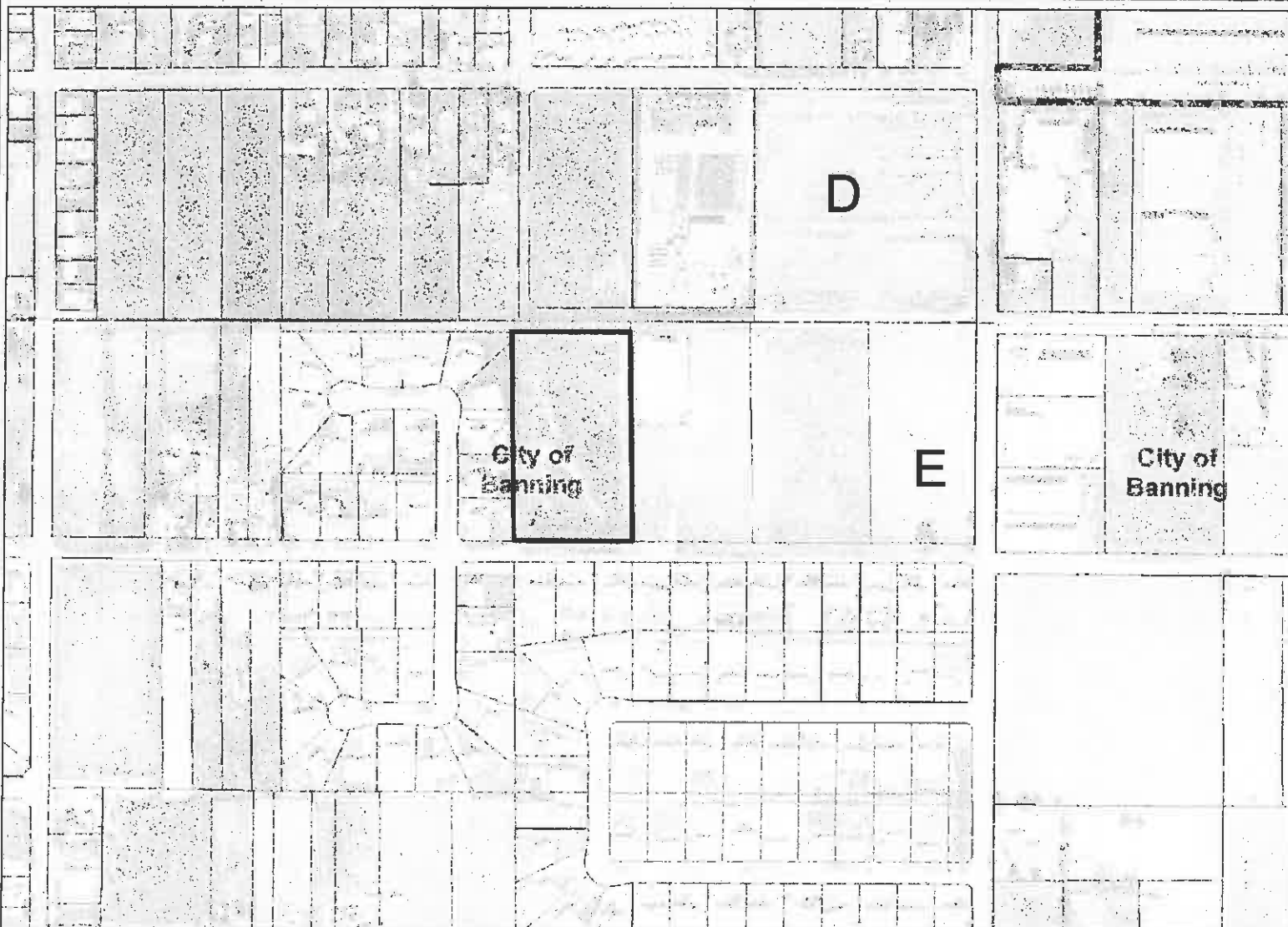
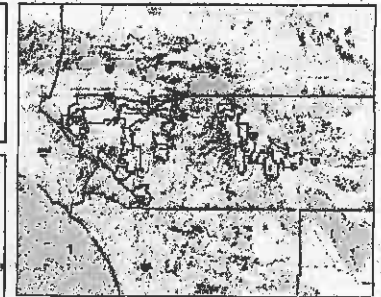
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/24/2016 2:06:46 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



0 517 1,033 Feet



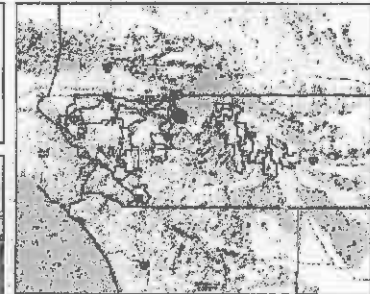
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/24/2016 2:05:19 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers

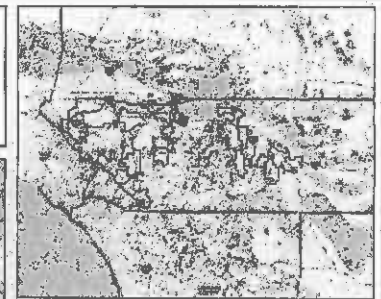


IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

Notes



My Map



Legend

- Display Parcels
- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 1,033 2,066 Feet



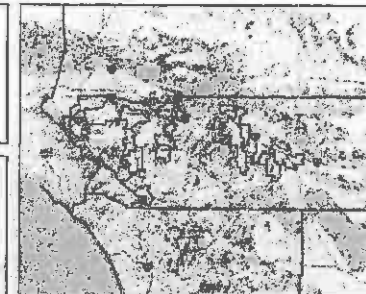
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/24/2016 2:09:08 PM







© Riverside County RCIT GIS

Notes

My Map



Legend

-  Display Parcels
-  City Boundaries
- Cities**
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
-  counties
-  cities
- hydrographylines
- waterbodies
-  Lakes
-  Rivers



0 517 1,033 Feet



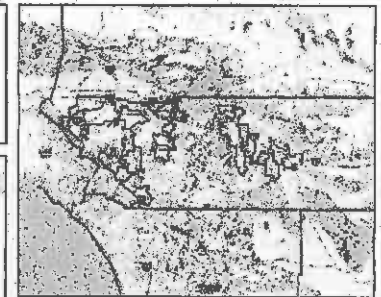
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/24/2016 2:06:02 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 258 517 Feet

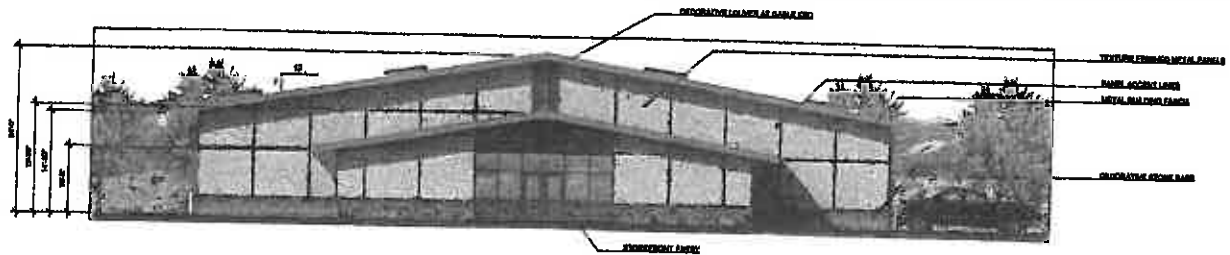


IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/24/2016 2:10:08 PM

© Riverside County RCIT GIS

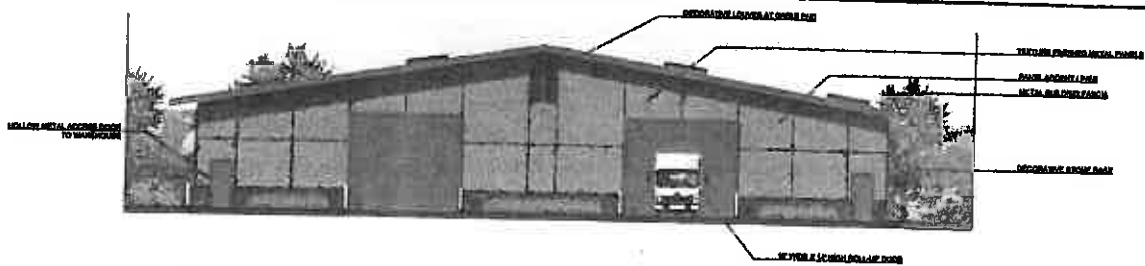
Notes



PRELIMINARY SOUTH EXTERIOR ELEVATION (FRONT)



PRELIMINARY EAST EXTERIOR ELEVATION (RIGHT SIDE)



PRELIMINARY NORTH EXTERIOR ELEVATION (REAR)



PRELIMINARY WEST EXTERIOR ELEVATION (LEFT SIDE)

INCORPORATED

HERRING ARCHITECTS, INC.
 28 SOUTH BAY STREET
 REDLANDS, CALIFORNIA 92371
 PH. (909) 775-6100



This plan and drawings are not to be copied, reproduced
 or employed for any other project without the written
 consent of Herring Architects, Inc.
 Copyright 2005

ANDERSEN RV & EQUIPMENT FACILITY
 ANDY AND REBEKAH ANDERSEN
 CLARK STREET (ADDRESS PENDING)
 BANNING, CALIFORNIA

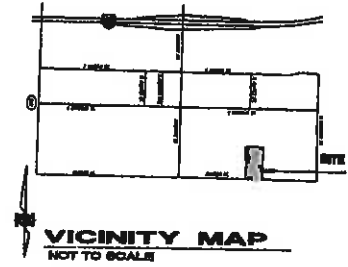
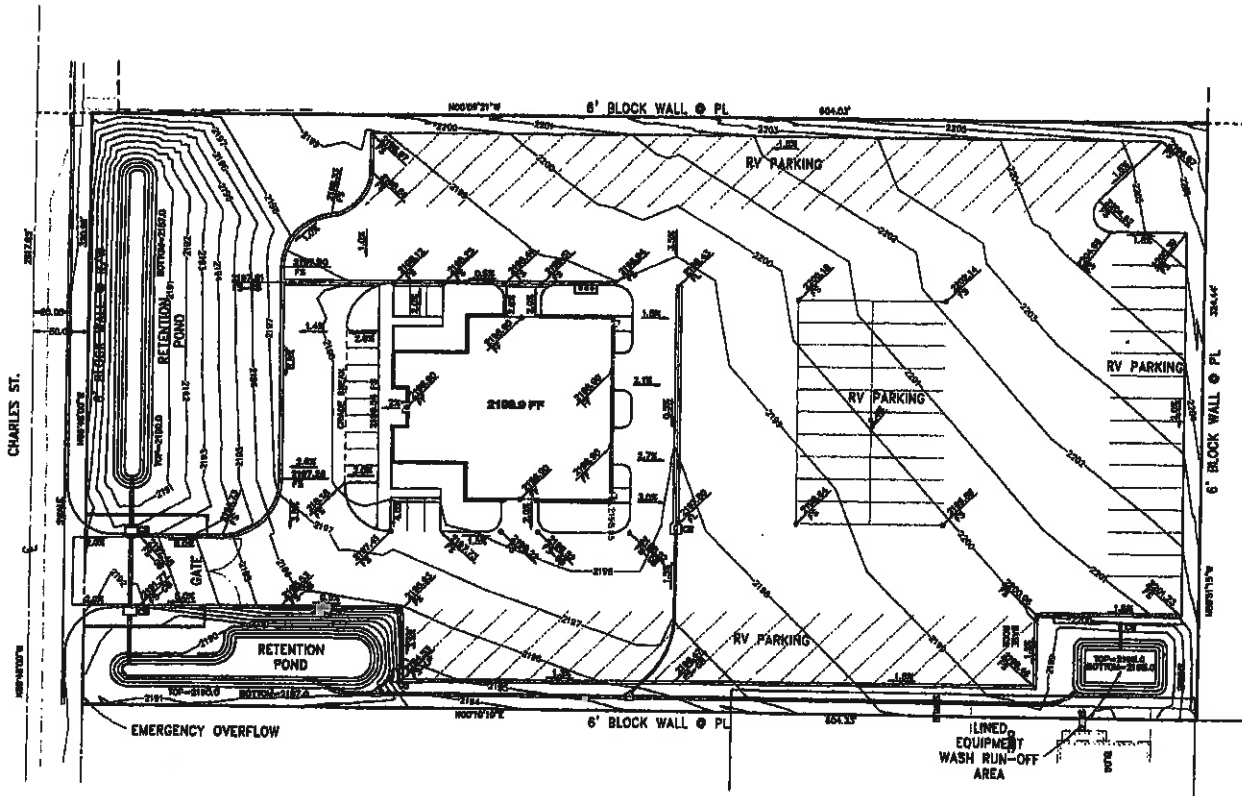
DATE: _____
 DATE: _____
 DATE: _____
 DRAWN BY: DWH
 CHECKED BY: P.A.
 DATE: 08/20/05

EXTERIOR ELEVATIONS

A-3

CONCEPTUAL GRADING PLAN

CITY OF BANNING, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA



PROPERTY DESCRIPTION

THE LAND REFERRED TO HEREIN IS SITUATED IN THE STATE OF CALIFORNIA, COUNTY OF RIVERSIDE, CITY OF BANNING AND DESCRIBED AS FOLLOWS:

THE WEST HALF OF BLOCK 63 OF BANNING COLONY LANDS IN THE CITY OF BANNING, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS SHOWN BY MAP ON FILE IN BOOK 5, PAGE 158 OF MAPS, RECORDS OF RIVERSIDE COUNTY, CALIFORNIA.

EXCEPTING THEREON, THAT PORTION CONVEYED TO THE CITY OF BANNING BY DEED RECORDED JUNE 20, 1977 AS INSTRUMENT NO. 77-122218 OF OFFICIAL RECORDS.

APN 543-090-019-5

ADDRESS

NO ADDRESS ON FILE

ADDRESS

DESCRIPTION	CUY	FEET
GRV	4,000	3,000
CLEAR & GRUB	1,000	1,000
BLDG OVER EX	1,000	1,000
PARKING OVER EX	1,000	1,000
SPRINKLER	100	254
SLAB ON GRADE		
TOTAL	8,344	8,254
SITE BALANCE		



DATE: 11/03/2015

CONTRACT DEVELOPER
Andy Anderson and
Pamela Anderson
3080 ELANE WAY
TUCUMPA, CA 92309

DATE	BY	REVISION	REV.
04/01/15	RH	15% RELEASE	001



PREPARED BY
PUBLA ENGINEERING, INC.
Planning & Engineering & Surveying
4790 IVINE BLVD. STE 200-202
IRVINE, CALIF. 92614
(951) 740-8042 • (951) 740-8041 FAX

PAGE BREAK



**AIRPORT LAND USE COMMISSION
RIVERSIDE COUNTY**



CHAIR
Simon Housman
Rancho Mirage

VICE CHAIRMAN
Rod Ballance
Riverside

COMMISSIONERS

Arthur Butler
Riverside

John Lyon
Riverside

Glen Holmes
Hemet

Steve Manos
Lake Elsinore

Russell Betts
Desert Hot Springs

STAFF

Director
Ed Cooper

John Guerin
Paul Rull
Barbara Santos

County Administrative Center
4080 Lamon St., 14th Floor
Riverside, CA 92501
(951) 955-6132

www.rcaluc.org

November 8, 2016

Ms. Judy Eguez, Project Planner

City of Riverside Community and Economic Development Department/Planning Division
3900 Main Street, Third Floor
Riverside, CA 92522

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –
DIRECTOR’S DETERMINATION**

File No.: ZAP1226MA16
Related File Nos.: P16-0766 (Conditional Use Permit)
APN: 249-110-064

Dear Ms. Eguez:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed City of Riverside Case No. P16-0766 (Conditional Use Permit), a proposal to establish a private university within approximately 14,000 square feet of an existing 5-story office building on a 2.81 acre parcel with an address of 1325 Spruce Street, located at the northwest corner of Iowa Avenue and Spruce Street.

The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, nonresidential intensity is not restricted.

No change to building height or construction of additional structures is proposed. Therefore, FAA OES review was not required for height/elevation reasons.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, subject to the following conditions:

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.

AIRPORT LAND USE COMMISSION

2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris centers, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The landowner shall provide the attached notice to all potential purchasers of the property and to tenants/lessees of the building thereon.

If you have any questions, please contact Paul Rull, ALUC Urban Regional Planner IV, at (951) 955-6893 or John Guerin, ALUC Principal Planner, at (951) 955-0982.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



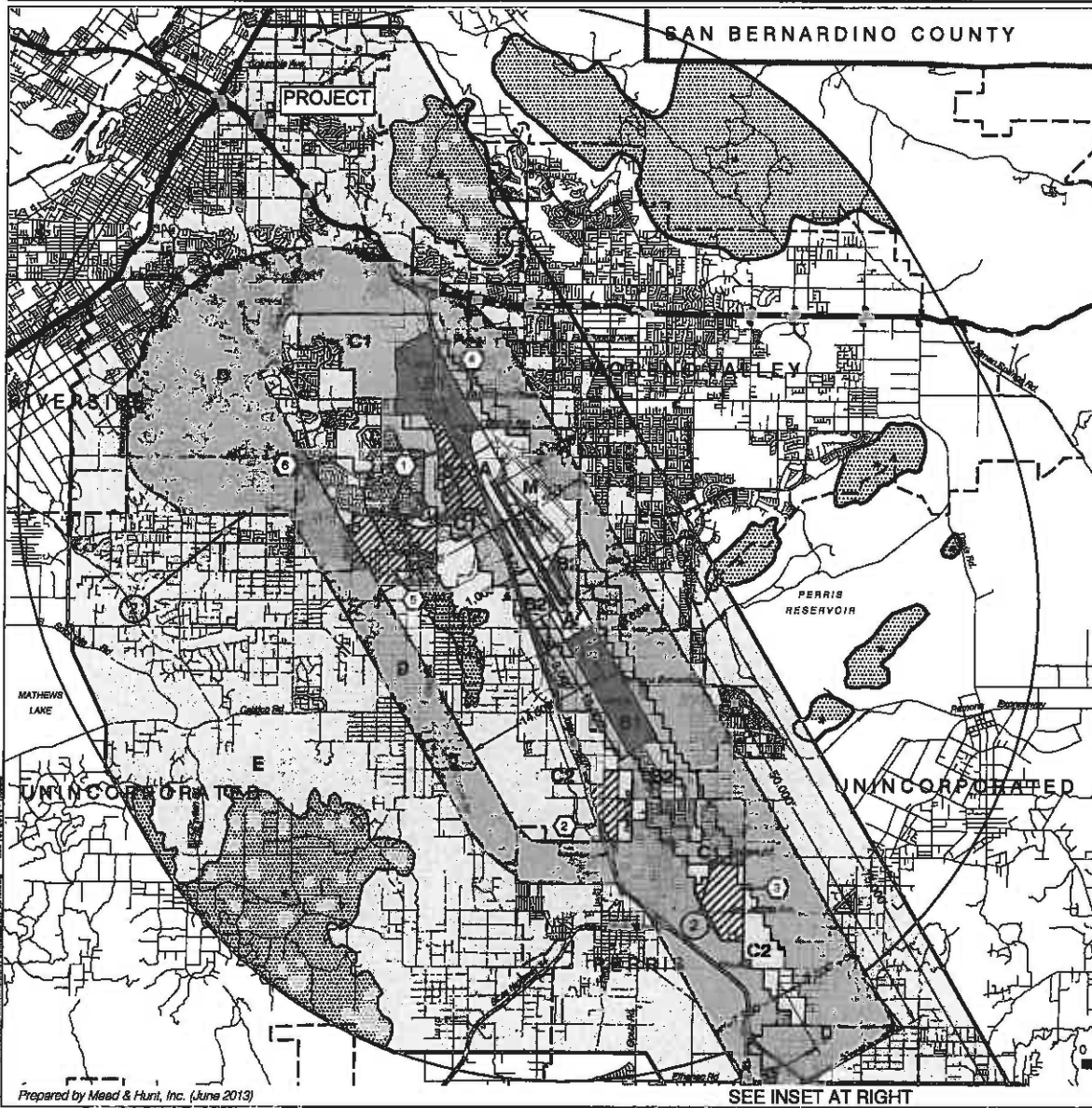
Edward C. Cooper, Director

Attachments: Notice of Airport in Vicinity

cc: David Stapley, The Magnon Companies/Spruce Professional Building LLC
(applicant/landowner/payee)
Carter Redish, Carter Group Architects (representative)
Gary Gosliga, Airport Manager, March Inland Port Airport Authority
Denise Hauser, March Air Reserve Base
ALUC Case File

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



LEGEND

Compatibility Zones

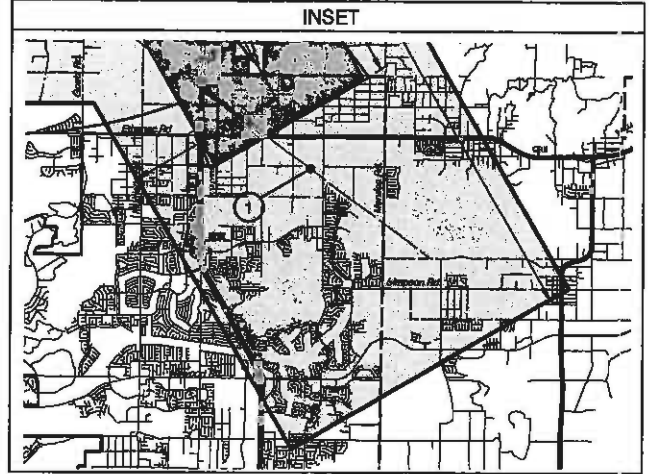
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- ▨ Site-Specific Exceptions (existing local agency commitments to development projects)

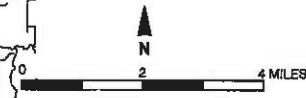
- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



**Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)**

Note:
All dimensions are measured from
runway ends and centerlines.



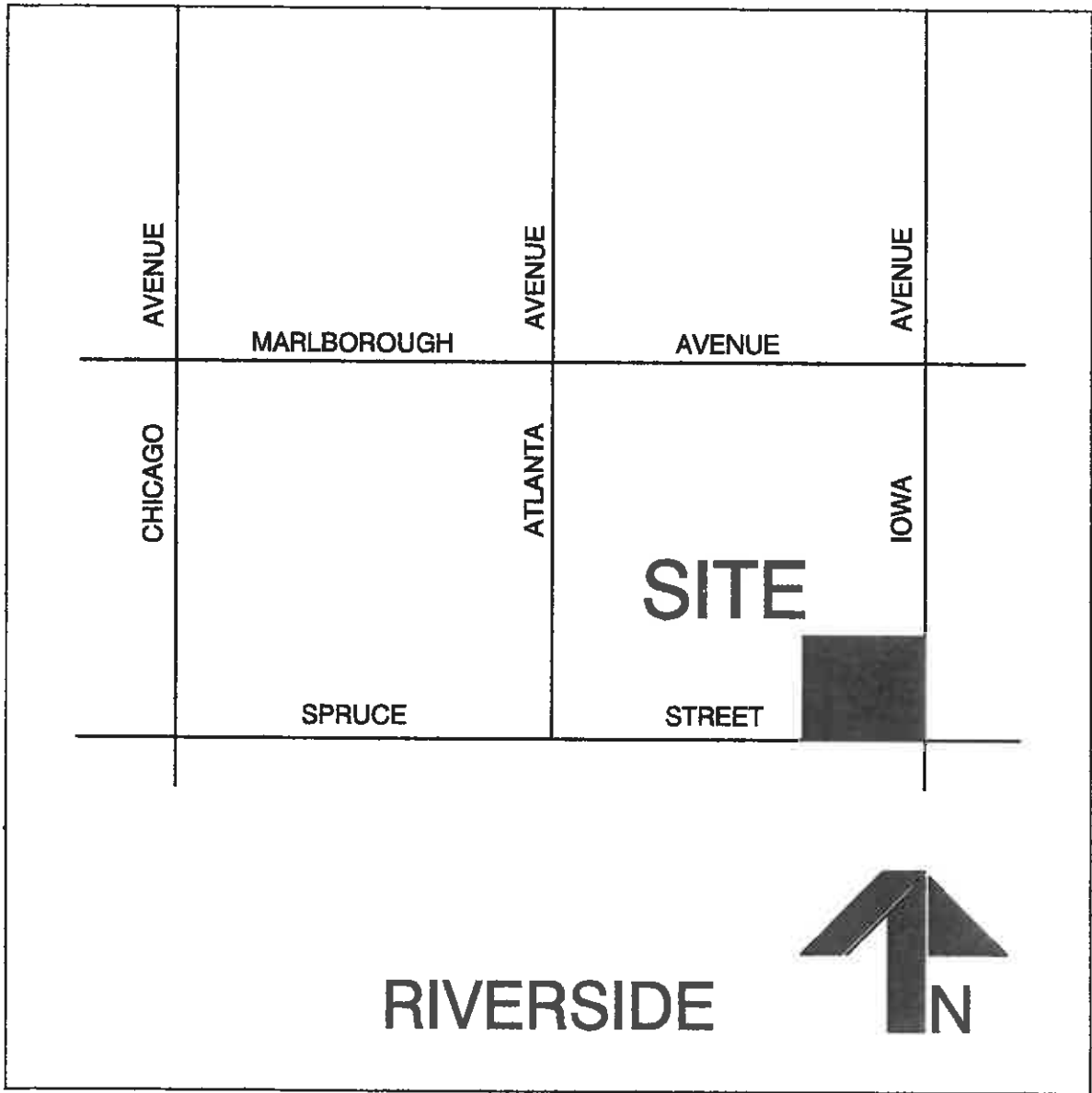
Base map source: County of Riverside 2013

Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

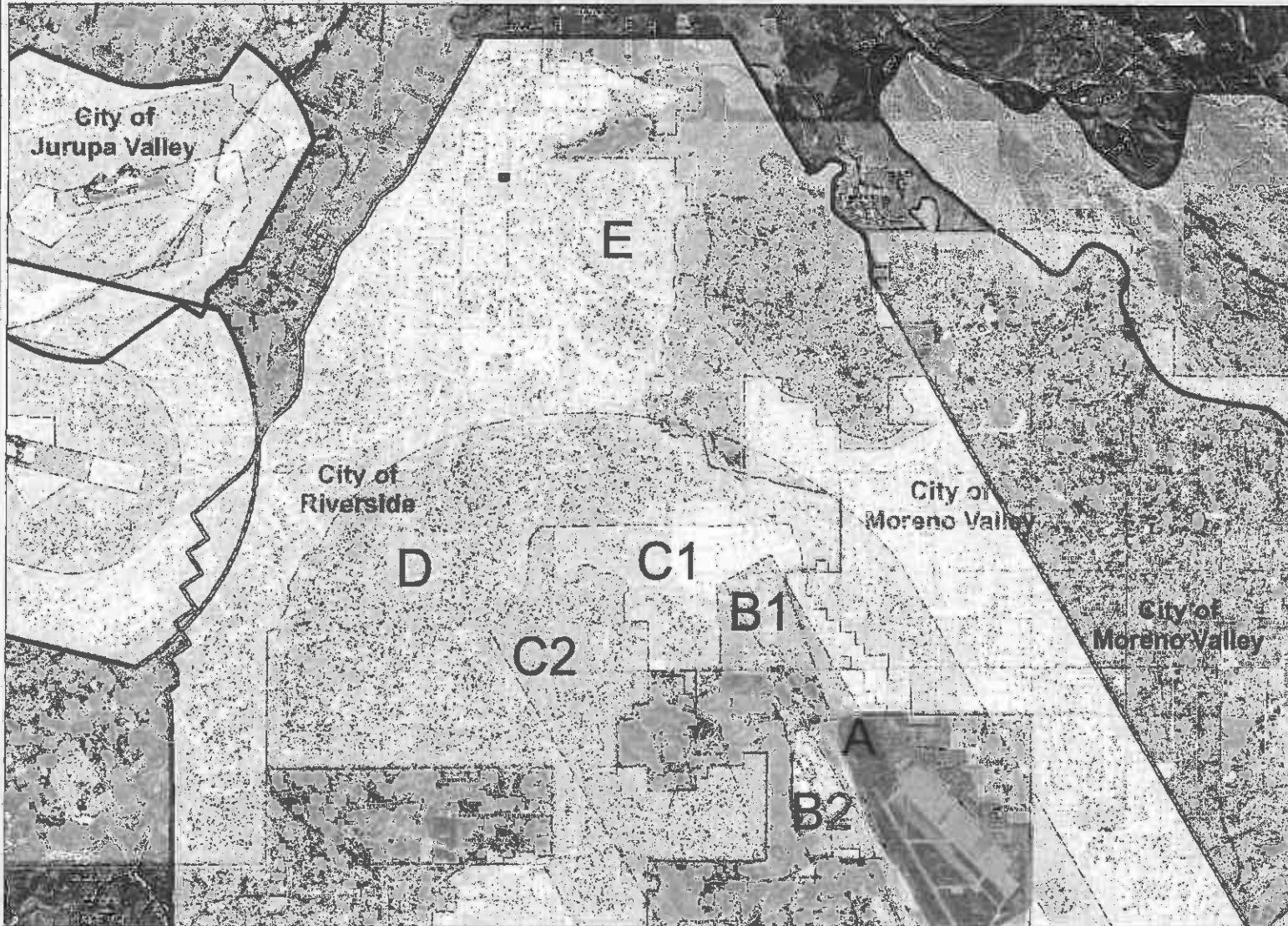
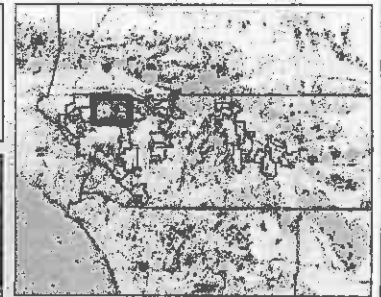
Map MA-1

Compatibility Map
March Air Reserve Base / Inland Port Airport



Vicinity Map

My Map



Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



0 10,310 20,620 Feet



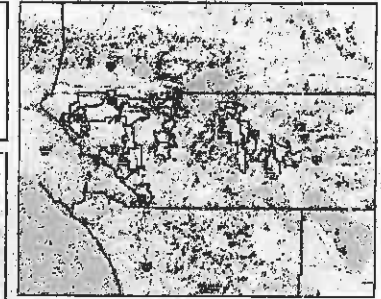
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/31/2016 3:26:34 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC8



0 322 644 Feet



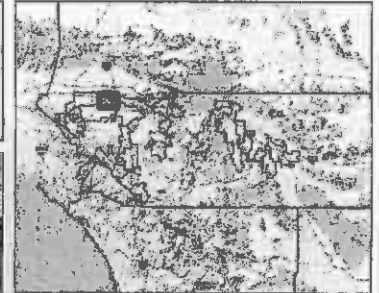
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/31/2016 3:24:53 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- City Boundaries
- Cities
- highways_large
 - HWY
 - INTERCHANGE
 - INTERSTATE
 - USHWY
- majorroads
- counties
- cities
- hydrographylines
- waterbodies
 - Lakes
 - Rivers



0 5,155 10,310 Feet



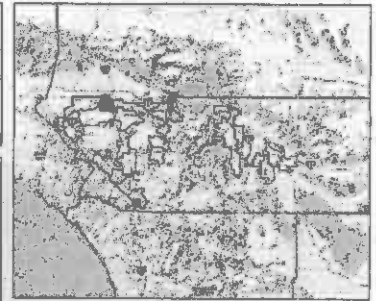
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/31/2016 3:27:12 PM






















© Riverside County RCIT GIS

Notes

My Map



Legend

-  City Boundaries
-  Cities
-  roads
-  highways
-  HWY
-  INTERCHANGE
-  INTERSTATE
-  OFFRAMP
-  ONRAMP
-  USHWY
-  roads
-  Major Roads
-  Arterial
-  Collector
-  Residential
-  counties
-  cities
-  hydrography
-  waterbodies
-  Lakes
-  Rivers



0 2,578 5,155 Feet



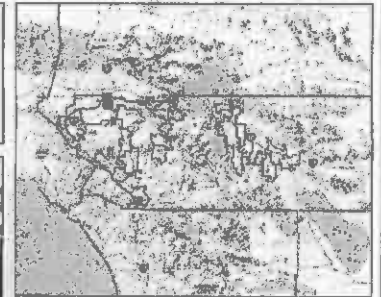
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/31/2016 3:27:37 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities
- roadsanno
- highways
 - HWY
 - INTERCHANGE
 - INTERSTATE
 - OFFRAMP
 - ONRAMP
 - USHWY
- counties
- cities
- hydrographylines
- waterbodies
 - Lakes
 - Rivers



0 1,289 2,578 Feet



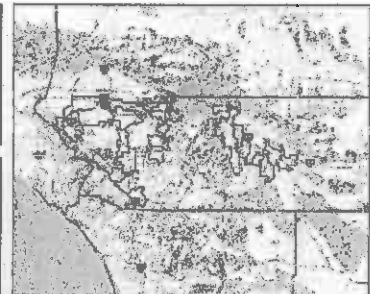
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/31/2016 3:28:08 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 644 1,289 Feet



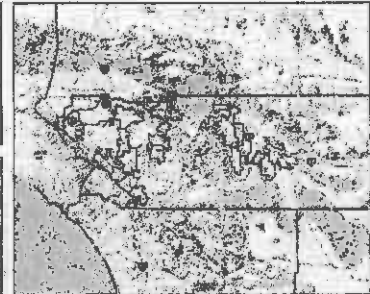
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/31/2016 3:28:39 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0

322

644 Feet



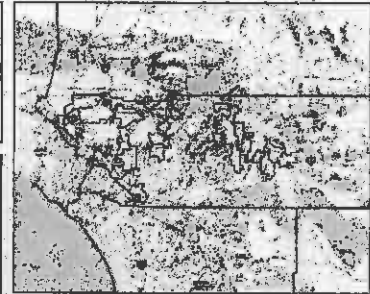
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/31/2016 3:25:33 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 161 322 Feet

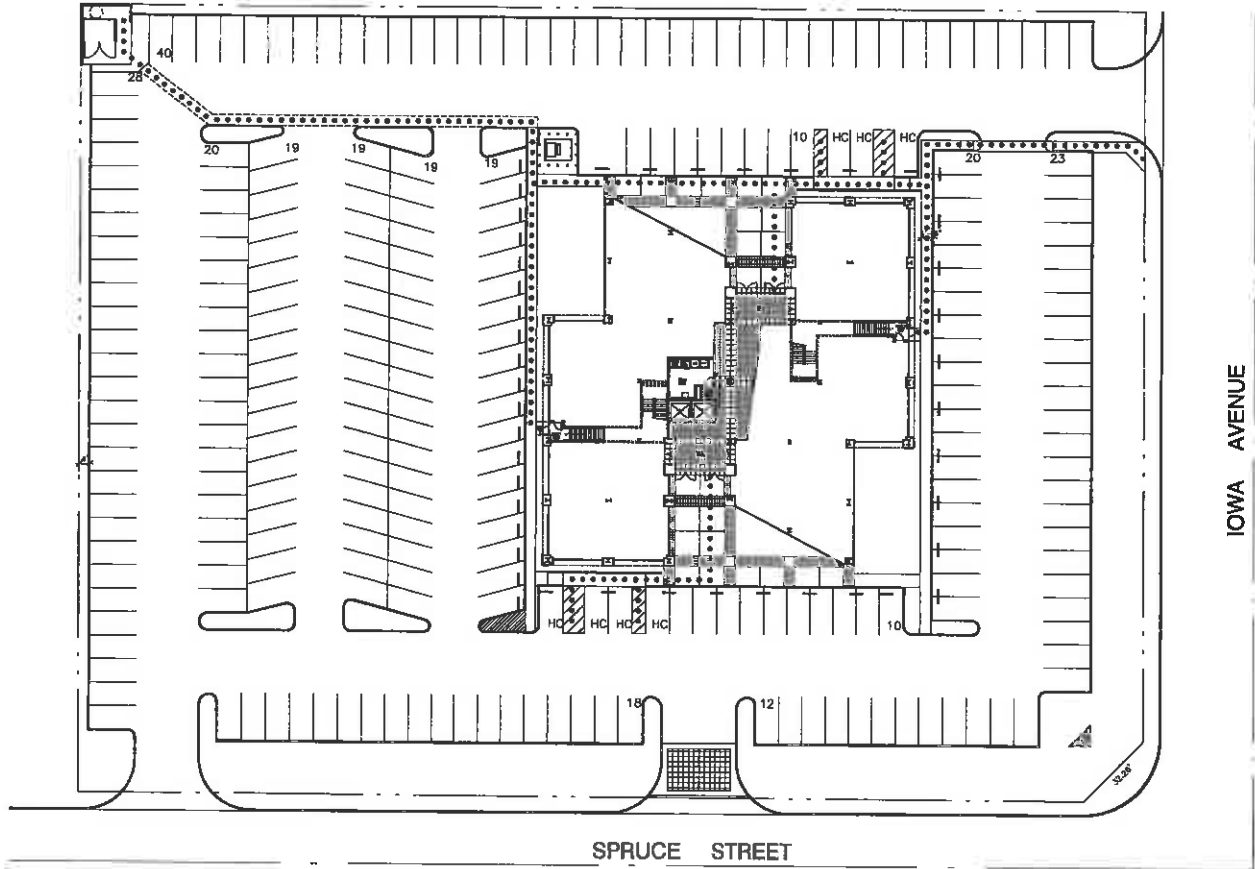


IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/31/2016 3:29:28 PM

© Riverside County RCIT GIS

Notes



Spruce Financial Center 2
1325 Spruce Street
Site Plan



BUSINESS OPERATIONS

11355 North Torrey Pines Road, La Jolla, CA 92037-1011
858-642-8593 • fax 858-642-8711 • www.nusystem.org

September 29, 2016

**Ted White, City Planner
City of Riverside
3900 Main Street, 3rd Floor
Riverside CA 92522**

Project Description:

To build and occupy a private university facility to serve the Riverside market area. The applicant National University will be establishing a classroom and support function facility at 1325 Spruce Street in Riverside. National University operates facilities in 10 locations in Southern California and has a total of 28 locations in the U.S. National University offers degree programs from undergraduate to graduate and credentials in areas to include education, letters and sciences, business and management, health and human services, and professional studies.

National University is requesting approval of a Conditional Use Permit to establish a Riverside operation of approximately 14,000 square feet within a five story office building located at 1325 Spruce Street. The space will be used for classrooms, administrative and support function offices and laboratories. National University will offer monthly onsite class sessions on weekdays and Saturdays as follows:

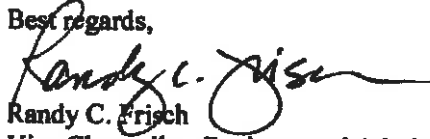
**Mondays – 6:00 PM to 10:00 PM
Tuesdays – 6:00 PM to 10:00 PM
Wednesdays – 6:00 PM to 10:00 PM
Thursdays – 6:00 PM to 10:00 PM
Saturdays – 8:30 AM to 12:30 PM and 1:00 PM to 5:00 PM**

The administrative offices will be open Monday thru Thursday from 8:00 AM to 10:00 PM, Fridays 8:00 AM to 6:00 PM and Saturdays 8:00 AM to 5:00 PM. National University will have no more than 25 full-time and 10 part-time staff members, consisting of administrators and instructors, at this location at a given time. Classes will generally contain 15 to 30 students and one instructor. Weekend sessions will be offered on Saturdays.

Parking for the school will be available on-site throughout the day for visitors as well as the students taking classes in the evening. Based on extensive experience, the parking needs for the proposed space will not be greater than 35 day time and 65 evening simultaneous spaces. This estimate is based on usage patterns at the many National University locations around Southern California to include typical utilization of public transit and car-pooling options.

With approval of this application, National University will be able to offer its highly regarded higher education programs to students throughout the Riverside area and add skilled graduates to the Riverside work force.

Best regards,

A handwritten signature in black ink that reads "Randy C. Frisch". The signature is written in a cursive style with a long horizontal flourish extending to the right.

Randy C. Frisch
Vice Chancellor, Business and Administration

PAGE BREAK



**AIRPORT LAND USE COMMISSION
RIVERSIDE COUNTY**



CHAIR
Simon Housman
Rancho Mirage

VICE CHAIRMAN
Rod Ballance
Riverside

COMMISSIONERS

Arthur Butler
Riverside

John Lyon
Riverside

Glen Holmes
Hemet

Steve Manos
Lake Elsinore

Russell Betts
Desert Hot Springs

STAFF

Director
Ed Cooper

John Guerin
Paul Rull
Barbara Santos

County Administrative Center
4080 Lamon St., 14th Floor
Riverside, CA 92501
(951) 955-5132

www.rcaluc.org

November 9, 2016

Ms. Lauren Sotelo, Project Planner
March Joint Powers Authority
14205 Meridian Parkway, Suite 140
Riverside CA 92518

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –
DIRECTOR’S DETERMINATION**

File No.: ZAP1224MA16
Related File No.: 16-01 (Plot Plan)
APN: 294-660-014

Dear Ms. Sotelo:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed March Joint Powers Authority Case No. 16-01 (Plot Plan), a proposal to construct a two-story 109,141 square foot acute care hospital with 156 beds on a 7.1 gross acre parcel, located northerly of N Street, westerly of 6th Street, easterly of Reservation Drive, and southerly of Myers Drive, northeasterly of the March runway within the land use jurisdiction of March Joint Powers Authority.

The site is located within Airport Compatibility Zone D of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone D of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, nonresidential intensity is not restricted.

The elevation of Runway 14-32 at March Air Reserve Base/Inland Port Airport at its northerly terminus is approximately 1,535 feet above mean sea level (AMSL). At a distance of 6,542 feet from the runway to the project, Federal Aviation Administration Obstruction Evaluation Services (FAA OES) review would be required for any structures with a top of roof exceeding 1,600.4 feet AMSL. The site’s finished floor elevation is 1,523.4 feet AMSL, and has a proposed maximum building height of 40 feet, resulting in a top point elevation of 1,563.4 feet AMSL. Although FAA OES is not technically required, the applicant did submit the project for FAA OES review and received a FAA Determination of No Hazard Letter dated January 7, 2016. The conditions of that letter have been incorporated into the attached ALUC conditions.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, subject to the following conditions:

AIRPORT LAND USE COMMISSION

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site.
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The landowner shall provide the attached notice to all potential purchasers of the property, which shall be recorded as a deed notice.
4. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment of irrigation controllers, access gates, etc.
5. The proposed detention basins on the site (including water quality management basins) shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the detention basin(s) shall not include trees that produce seeds, fruits, or berries.

AIRPORT LAND USE COMMISSION

6. The Federal Aviation Administration has conducted an aeronautical study of the proposed structures (Aeronautical Study No.2015-AWP-11896-OE) and has determined that neither marking nor lighting of the structures is necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 L and shall be maintained in accordance therewith for the life of the project.
7. The specific coordinates of the proposed structures shall not be amended and the top point elevation shall not be increased to a level greater than 1,564 feet above mean sea level without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in structure height or elevation shall not require further review by the Airport Land Use Commission.
8. Temporary construction equipment used during actual construction of the structures shall not exceed the height of the structures, unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.
9. Within five (5) days after construction of the structures reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned or a decision is made not to construct the structures.

If you have any questions, please contact Paul Rull, ALUC Urban Regional Planner IV, at (951) 955-6893 or John Guerin, ALUC Principal Planner, at (951) 955-0982.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Edward C. Cooper, Director

Attachments: Notice of Airport in Vicinity

cc: Signature Health Care, LLC, Erik Tolonen (applicant)
Riverside Life Properties, LLC (property owner/payee)
Albert A Webb Associates, Robert Berndt (representative)
Gary Gosliga, Airport Manager, March Inland Port Airport Authority
Denise Hauser, March Air Reserve Base
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1224MA16\ZAP1224MA16.LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2015-AWP-11896-OE

Issued Date: 01/07/2016

Mike Paulson
Gregg Maedo + Associates
321 N. Rampart Street
Suite 101
Orange, CA 92868

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Building Riverside Psychiatric Hospital
Location:	Riverside, CA
Latitude:	33-53-58.52N NAD 83
Longitude:	117-14-51.37W
Heights:	1520 feet site elevation (SE) 40 feet above ground level (AGL) 1560 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed and maintained in accordance with FAA Advisory circular 70/7460-1 L.

This determination expires on 07/07/2017 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates , heights, frequency(ies) and power . Any changes in coordinates , heights, and frequencies or use of greater power will void this determination. Any future construction or alteration , including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

If we can be of further assistance, please contact our office at (425) 227-2625. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2015-AWP-11896-OE.

Signature Control No: 274795209-277017689

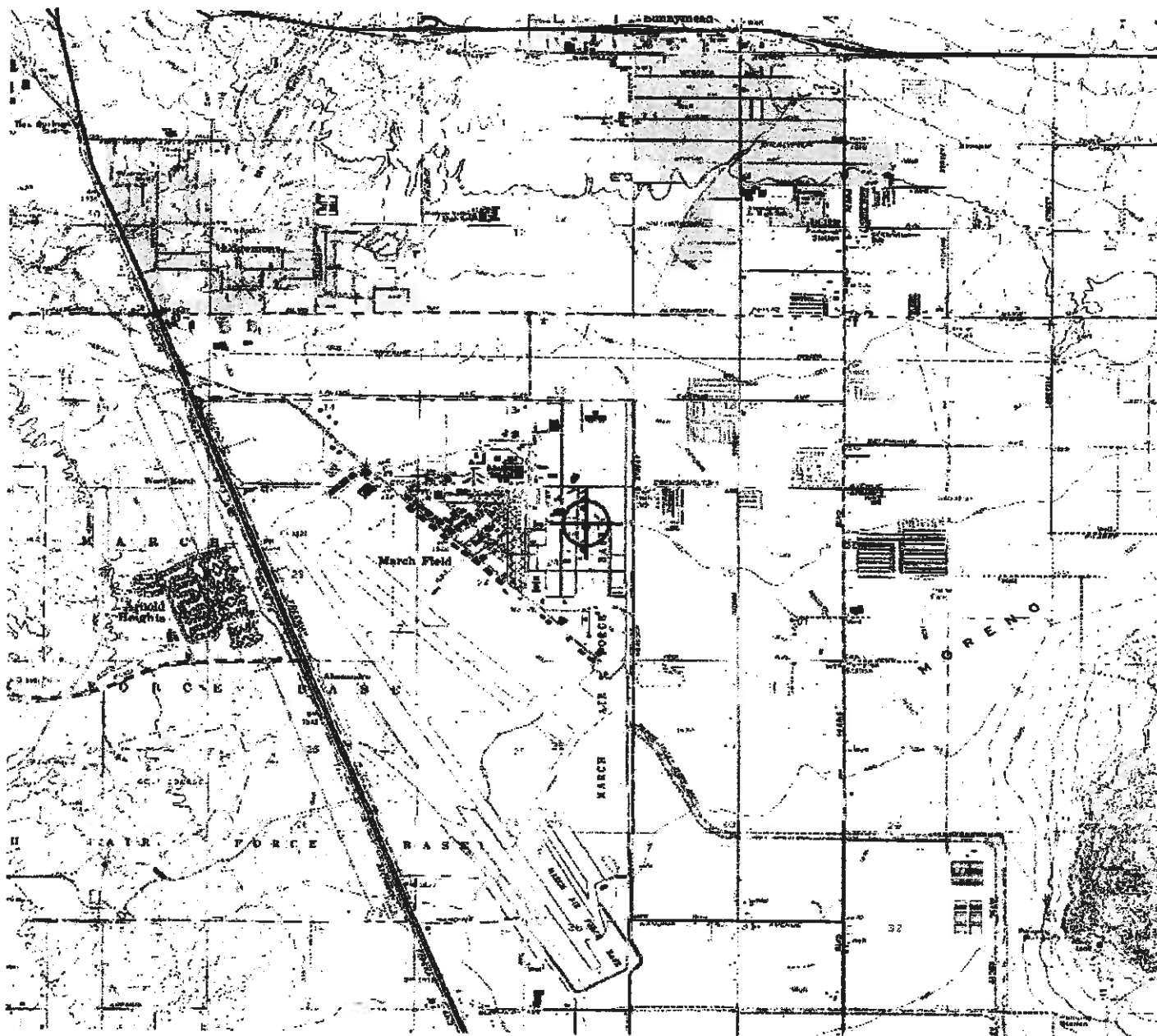
Paul Holmquist

Technician

(DNE)

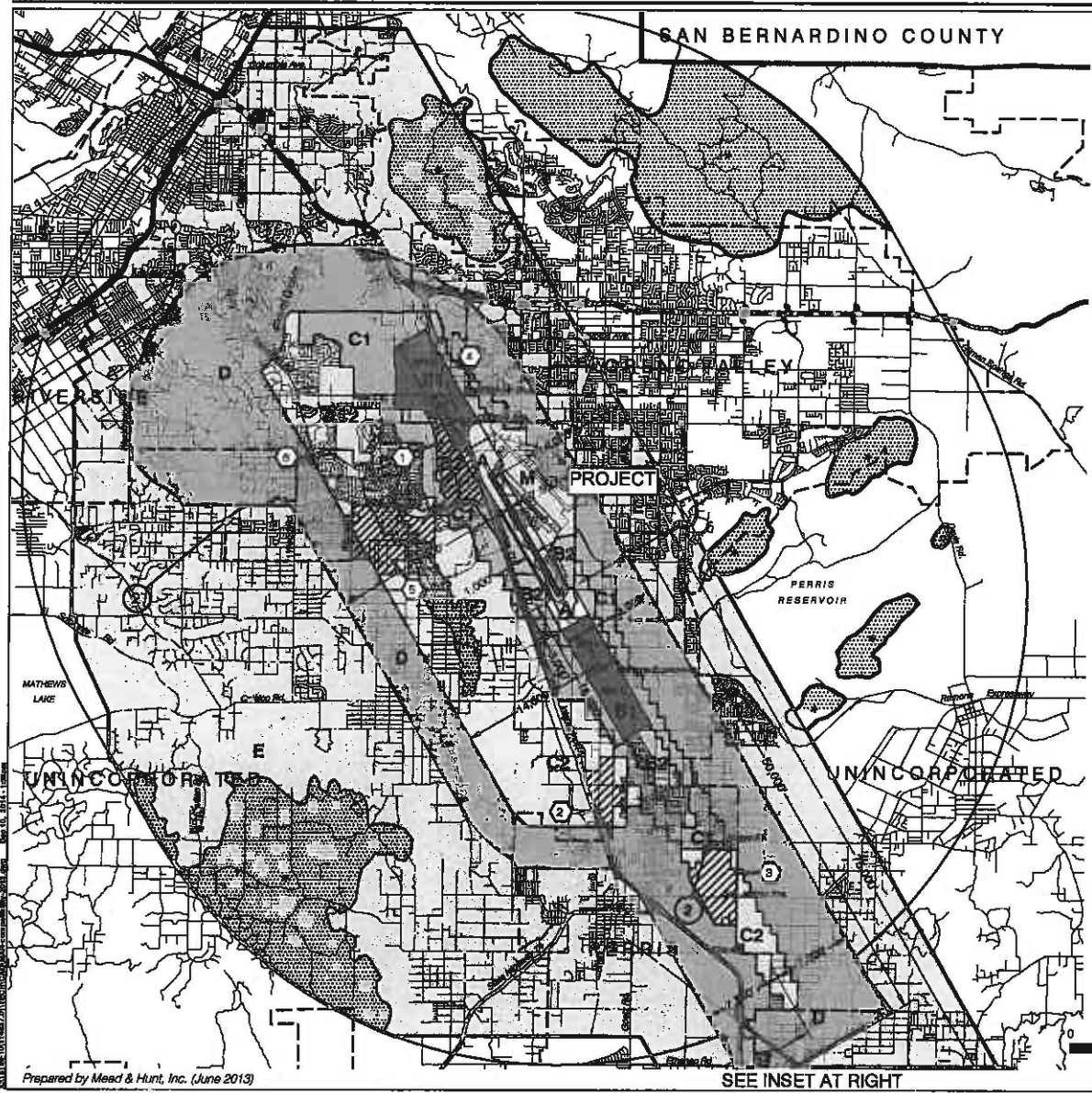
Attachment(s)

Map(s)



Sectional Map for ASN 2015-AWP-11896-OE





LEGEND

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

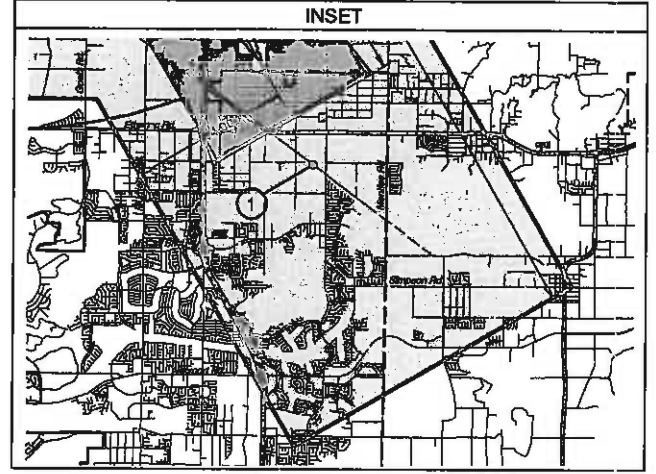
Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- ▣ Site-Specific Exceptions (existing local agency commitments to development projects)

① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.

② Point at which departing aircraft typically reach 3,000 feet above runway end.

① March JPA: March Business Center/Meridian
 ② Perris: Harvest Landing
 ③ Perris: Park West
 ④ Moreno Valley: Affordable Housing
 ⑤ March JPA: Ben Clark Training Center
 ⑥ Riverside: Ridge Crest Subdivision



**Riverside County
 Airport Land Use Commission
 March Air Reserve Base / Inland Port Airport
 Land Use Compatibility Plan
 (Adopted November 13, 2014)**

Note:
 All dimensions are measured from runway ends and centerlines.

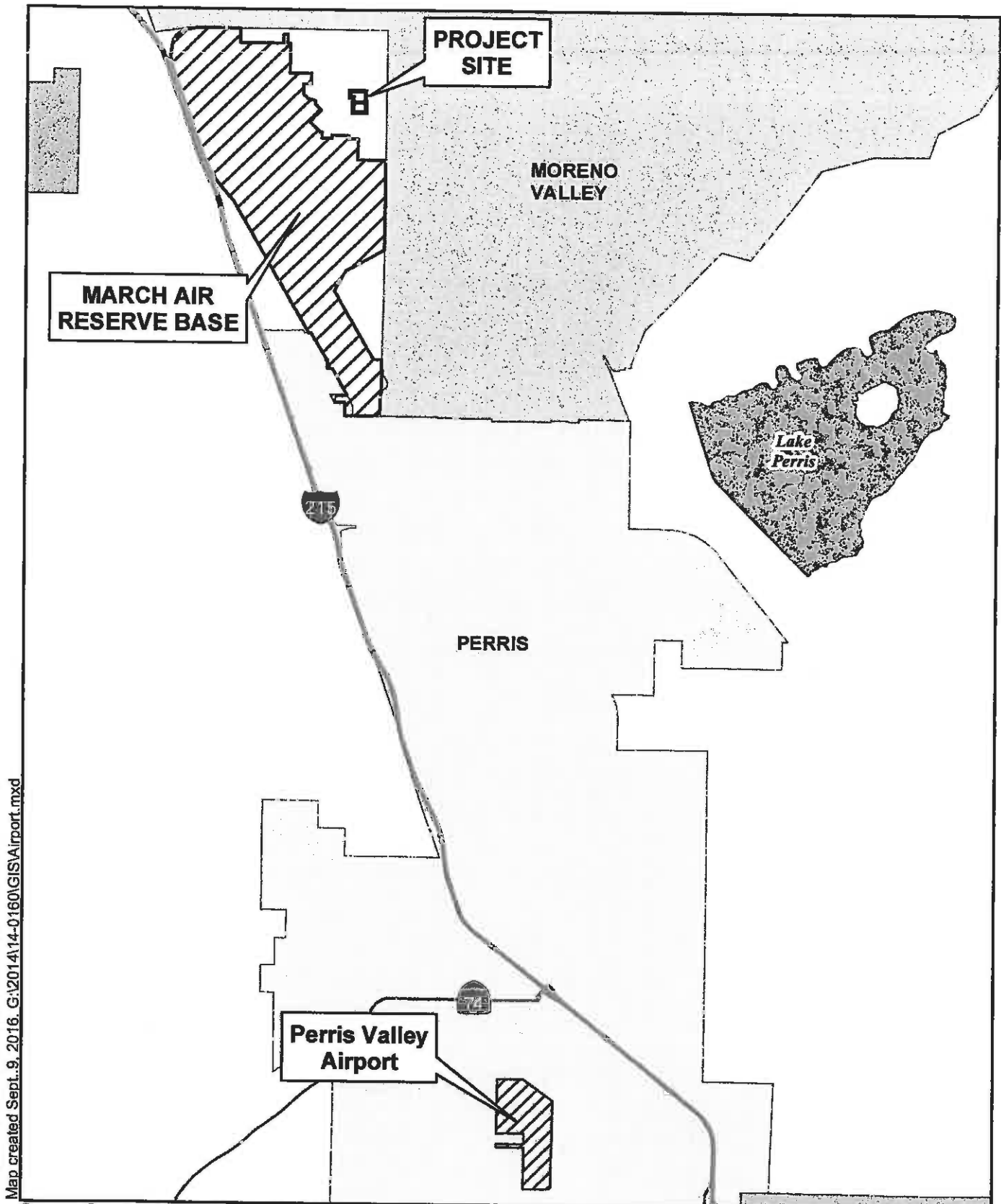


Base map source: County of Riverside 2013

Map MA-1
Compatibility Map
 March Air Reserve Base / Inland Port Airport

Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT



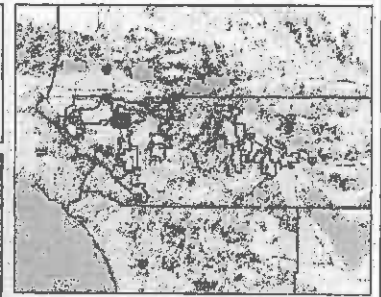
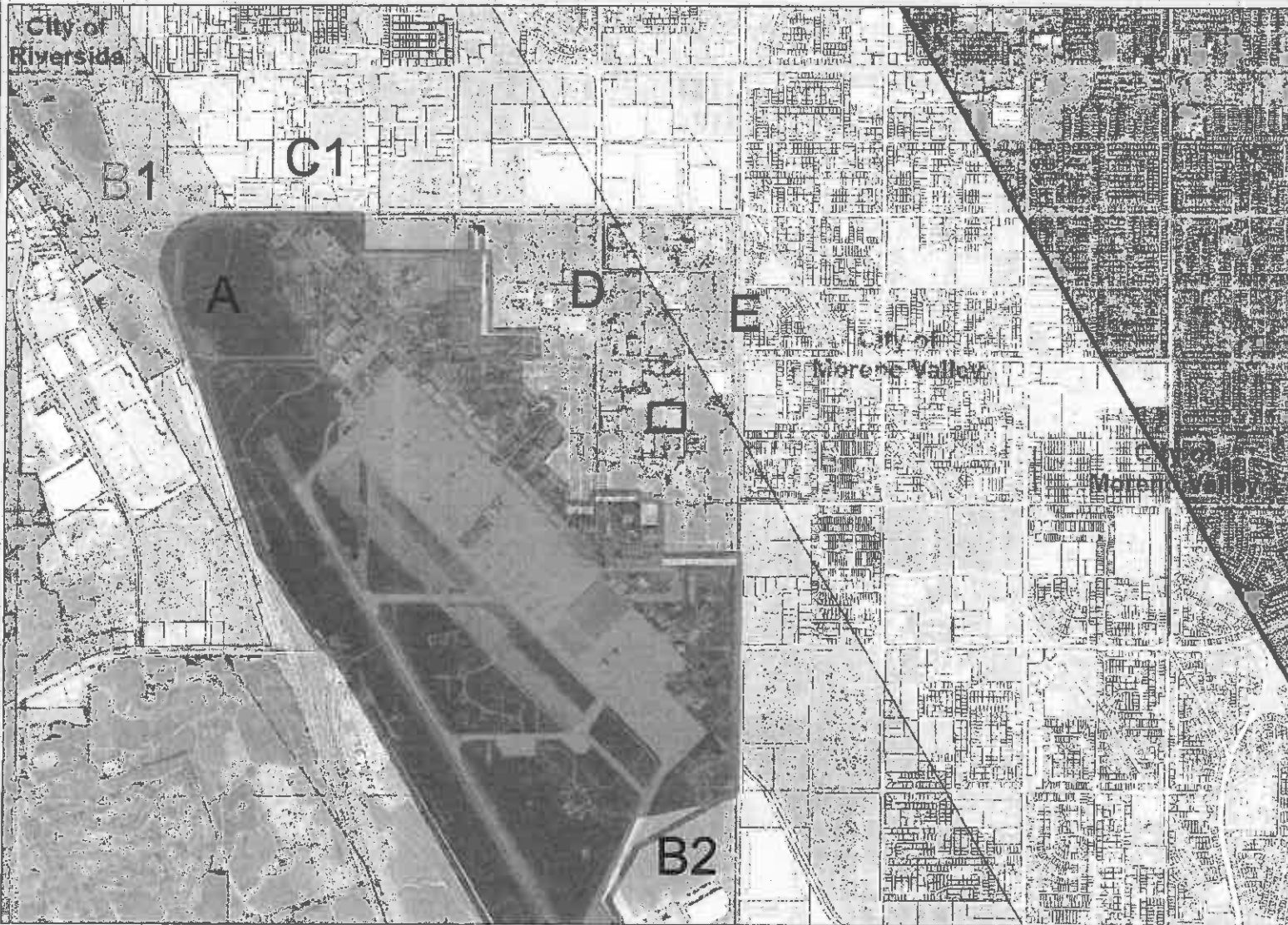
Map created Sept. 9, 2016. G:\2014\14-0160\GIS\Airport.mxd

Source: County of Riverside GIS, 2016

Relation to Nearby Airports
Aurora Behavioral Healthcare Hospital



My Map



Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



0 3,417 6,834 Feet



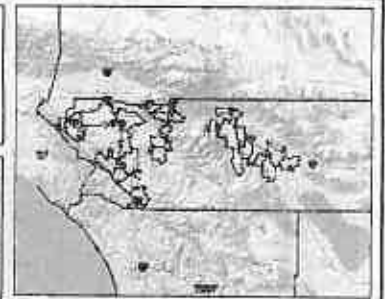
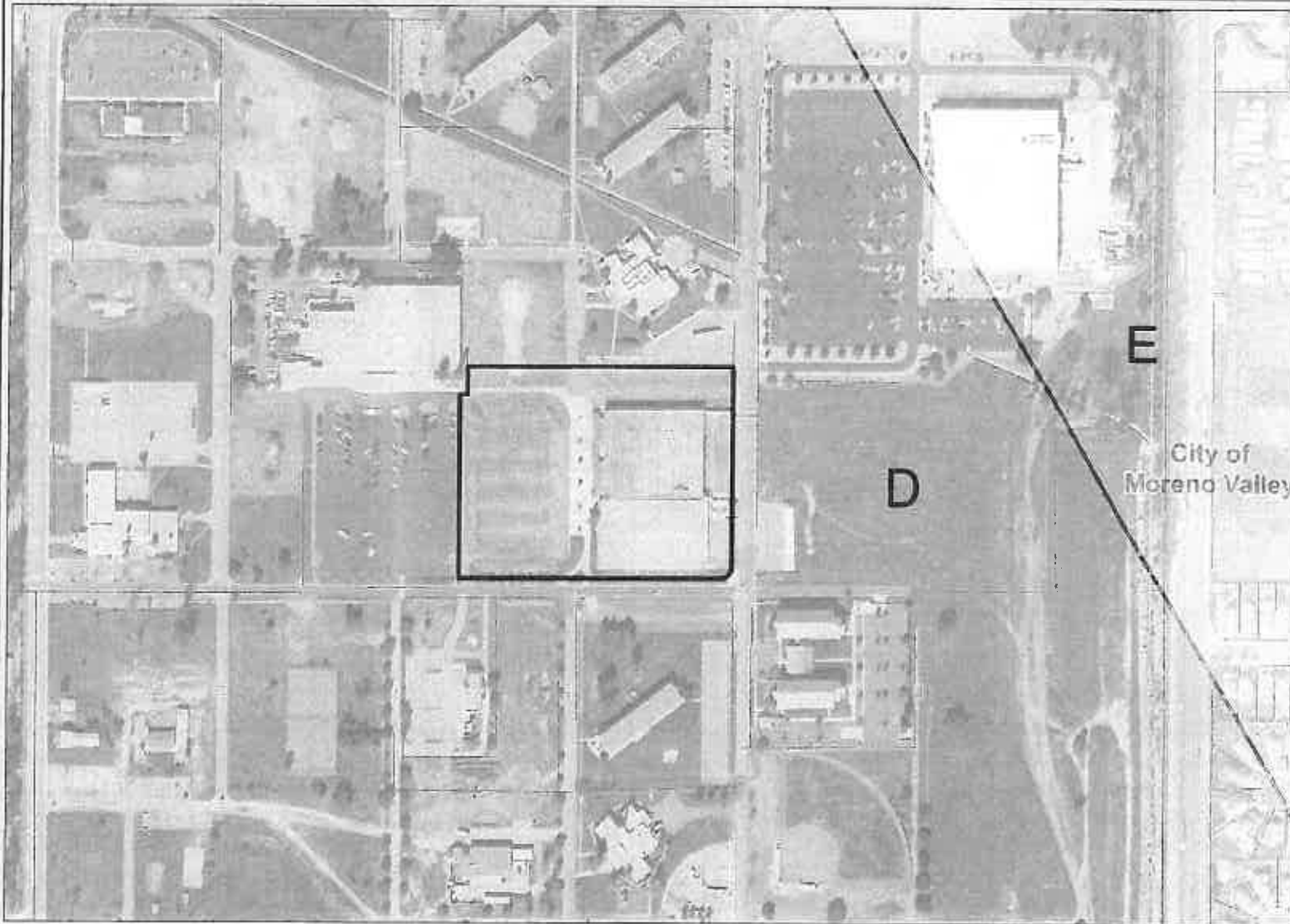
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/2/2016 9:02:27 AM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



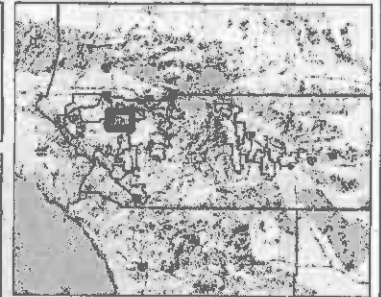
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/2/2016 9:00:55 AM

© Riverside County RCIT GIS

Notes

My Map



Legend

- City Boundaries
- Cities
- highways_large
 - HWY
 - INTERCHANGE
 - INTERSTATE
 - USHWY
- majorroads
- counties
- cities



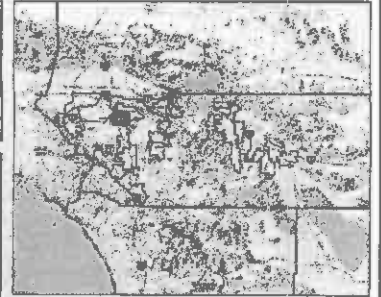
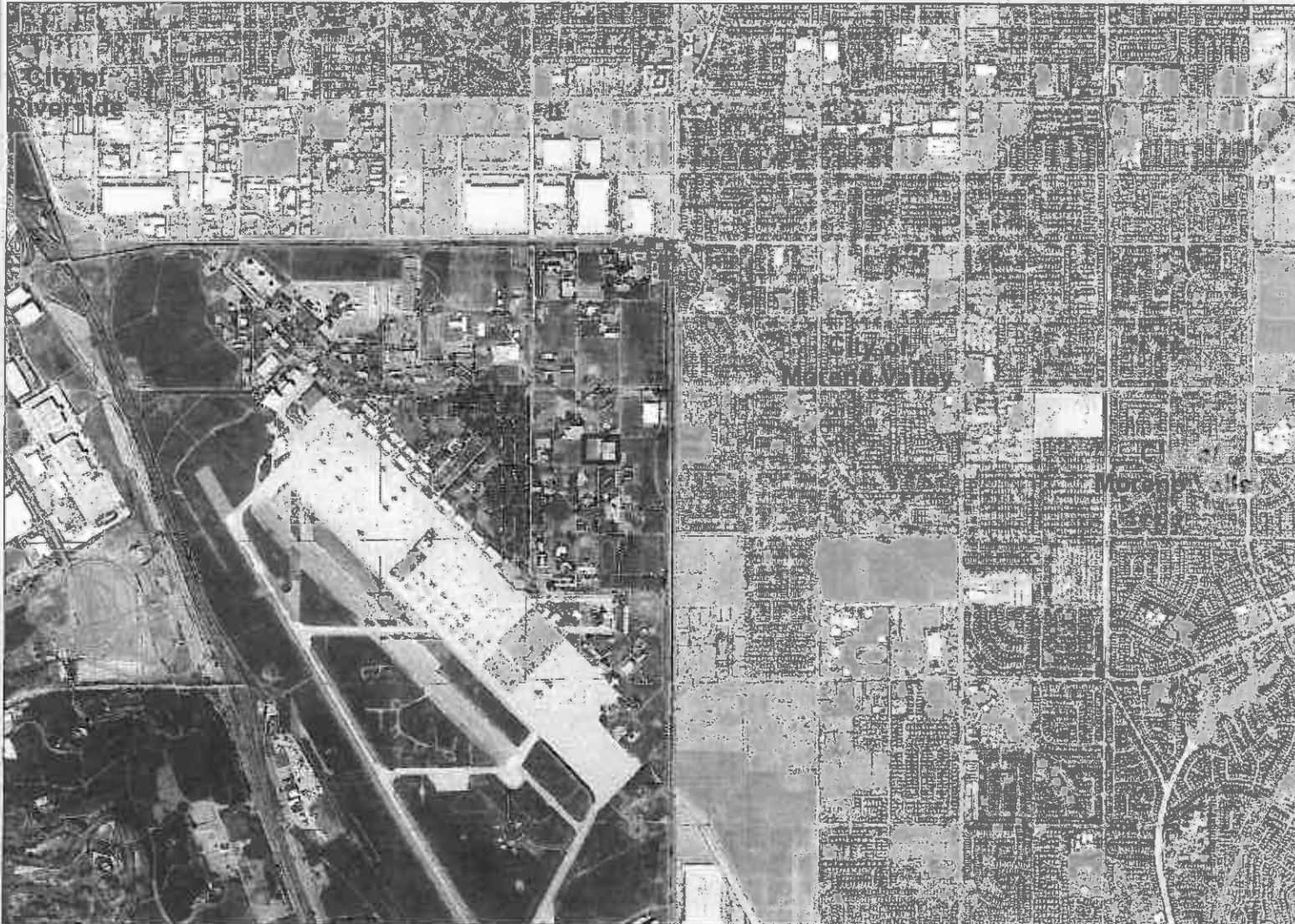
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/2/2016 9:03:18 AM
















© Riverside County RCIT GIS

Notes

My Map



Legend

-  City Boundaries
- Cities
- roadsanno
- highways
 -  HWY
 -  INTERCHANGE
 -  INTERSTATE
 -  OFFRAMP
 -  ONRAMP
 -  USHWY
- roads
 -  Major Roads
 -  Arterial
 -  Collector
 -  Residential
-  counties
-  cities
- hydrographylines
- waterbodies
 -  Lakes
 -  Rivers



0 3,417 6,834 Feet



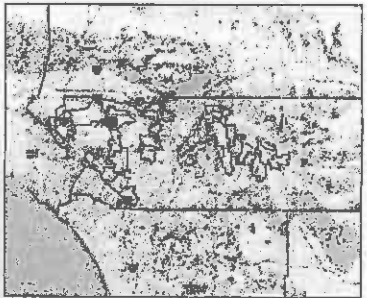
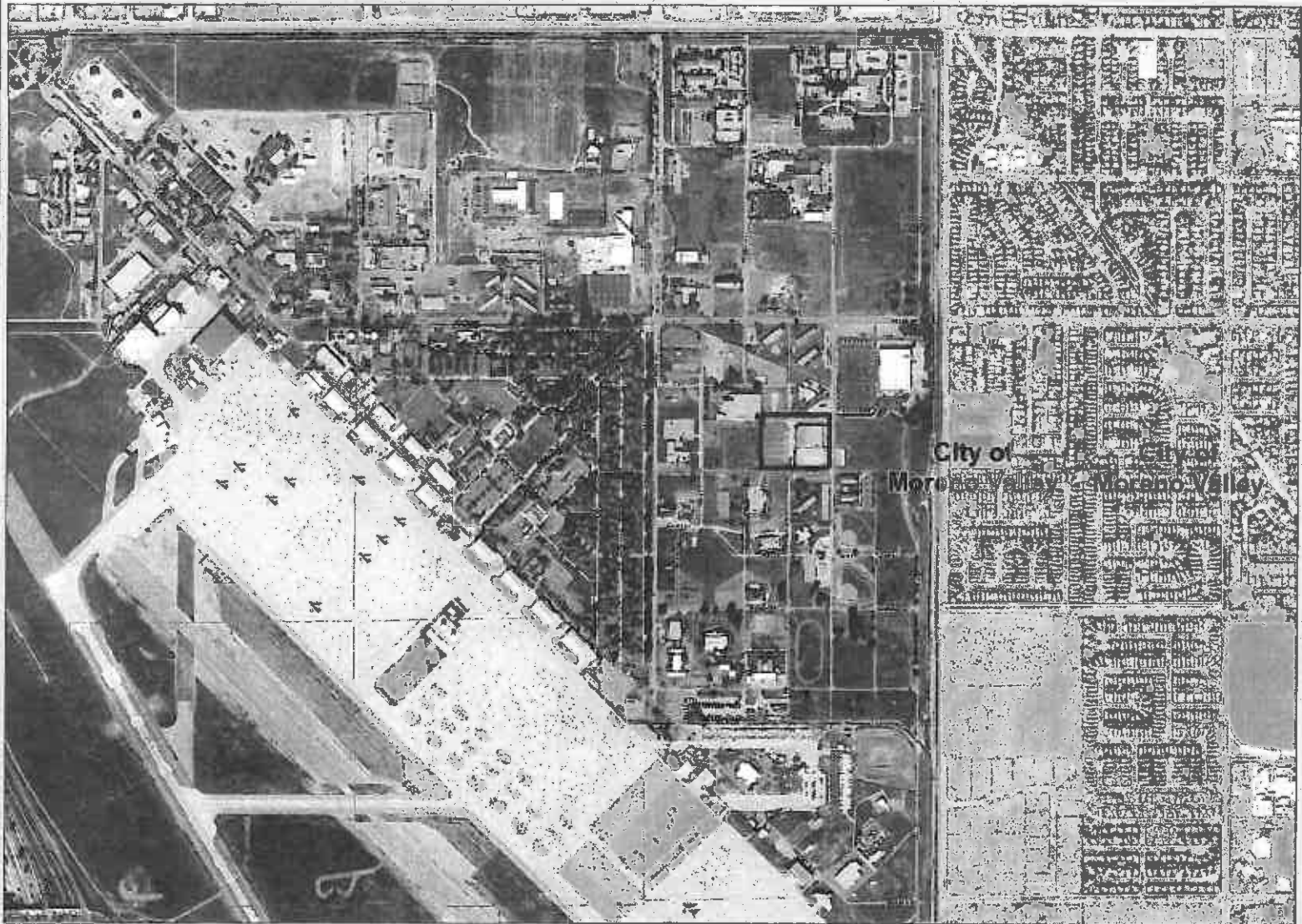
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/2/2016 9:03:51 AM

© Riverside County RCIT GIS

Notes

My Map



Legend

- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 1,708 3,417 Feet



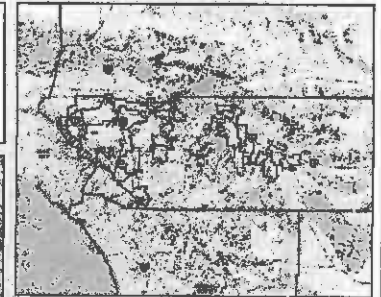
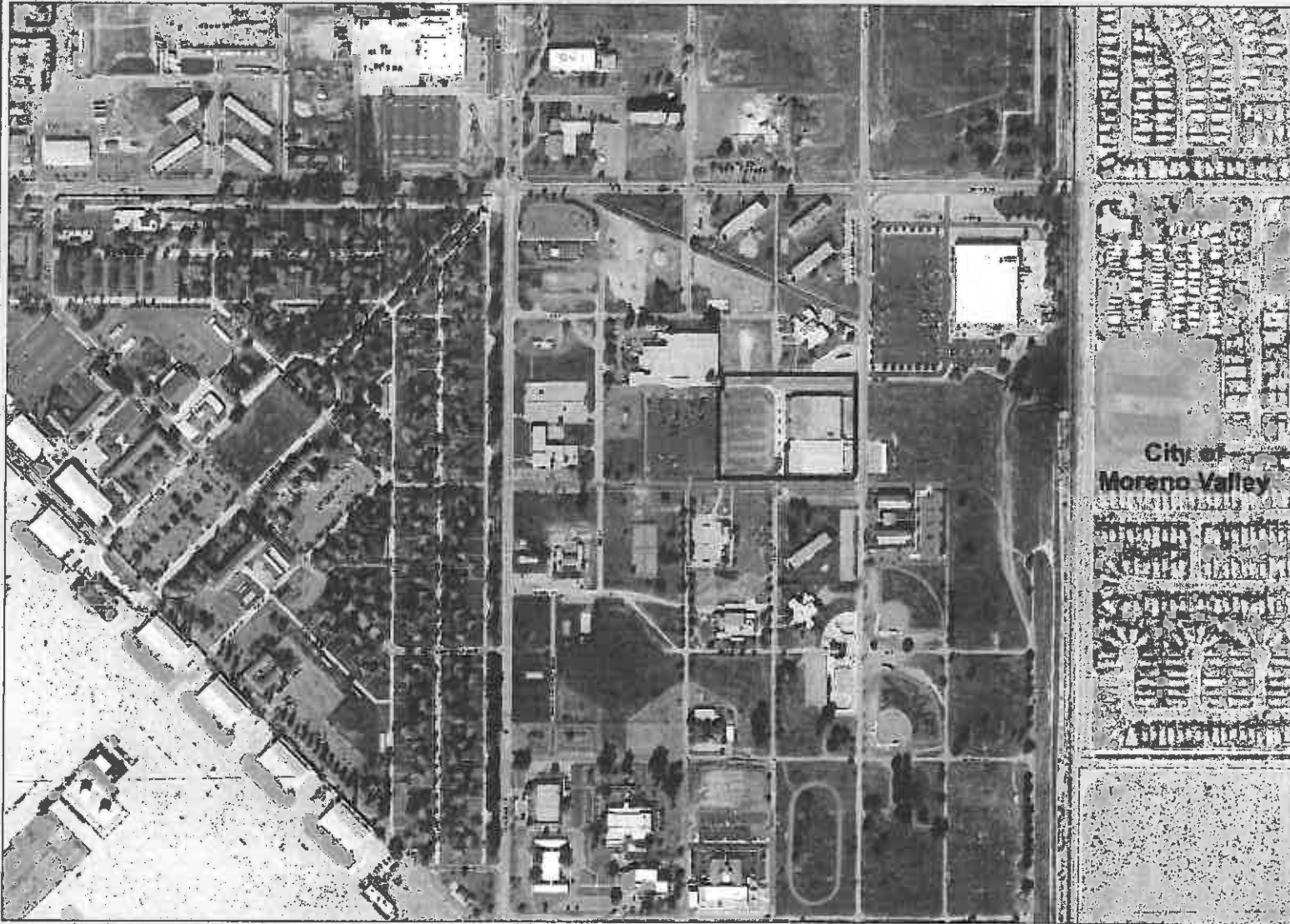
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/2/2016 9:04:27 AM

© Riverside County RCIT GIS

Notes

My Map



Legend

- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 854 1,708 Feet



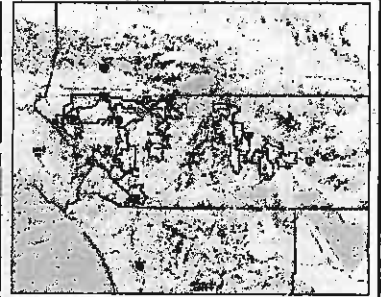
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/2/2016 9:04:58 AM






© Riverside County RCIT GIS

Notes

My Map



Legend

-  City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
-  counties
-  cities
- hydrographylines
- waterbodies
-  Lakes
-  Rivers



0 427 854 Feet



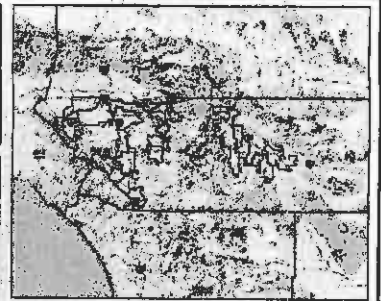
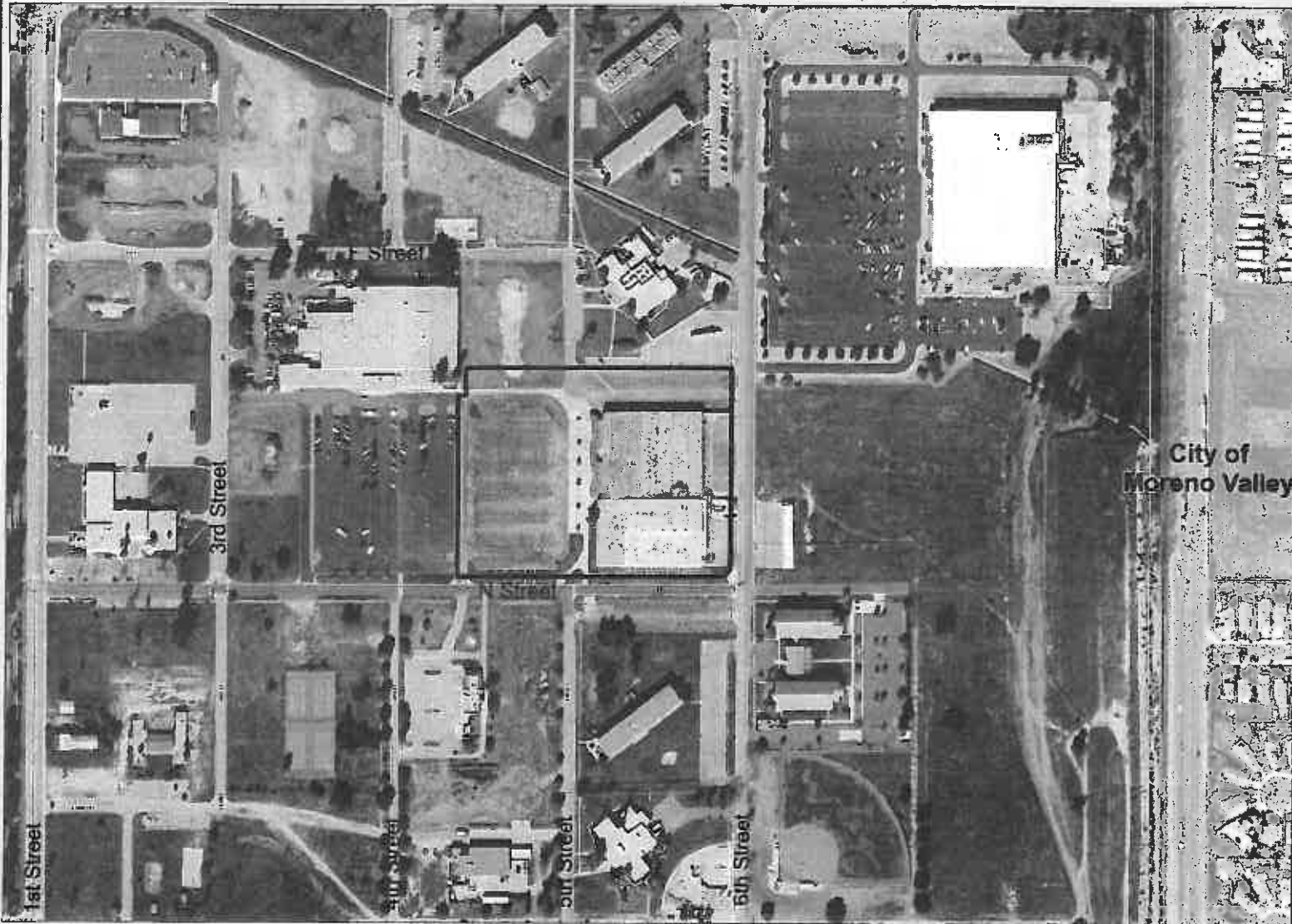
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/2/2016 9:05:38 AM



















© Riverside County RCIT GIS

Notes

My Map



Legend

-  City Boundaries
-  Cities
-  roads
-  anno
-  highways
-  HWY
-  INTERCHANGE
-  INTERSTATE
-  OFFRAMP
-  ONRAMP
-  USHWY
-  counties
-  cities
-  hydrography
-  lines
-  waterbodies
-  Lakes
-  Rivers



0 427 854 Feet



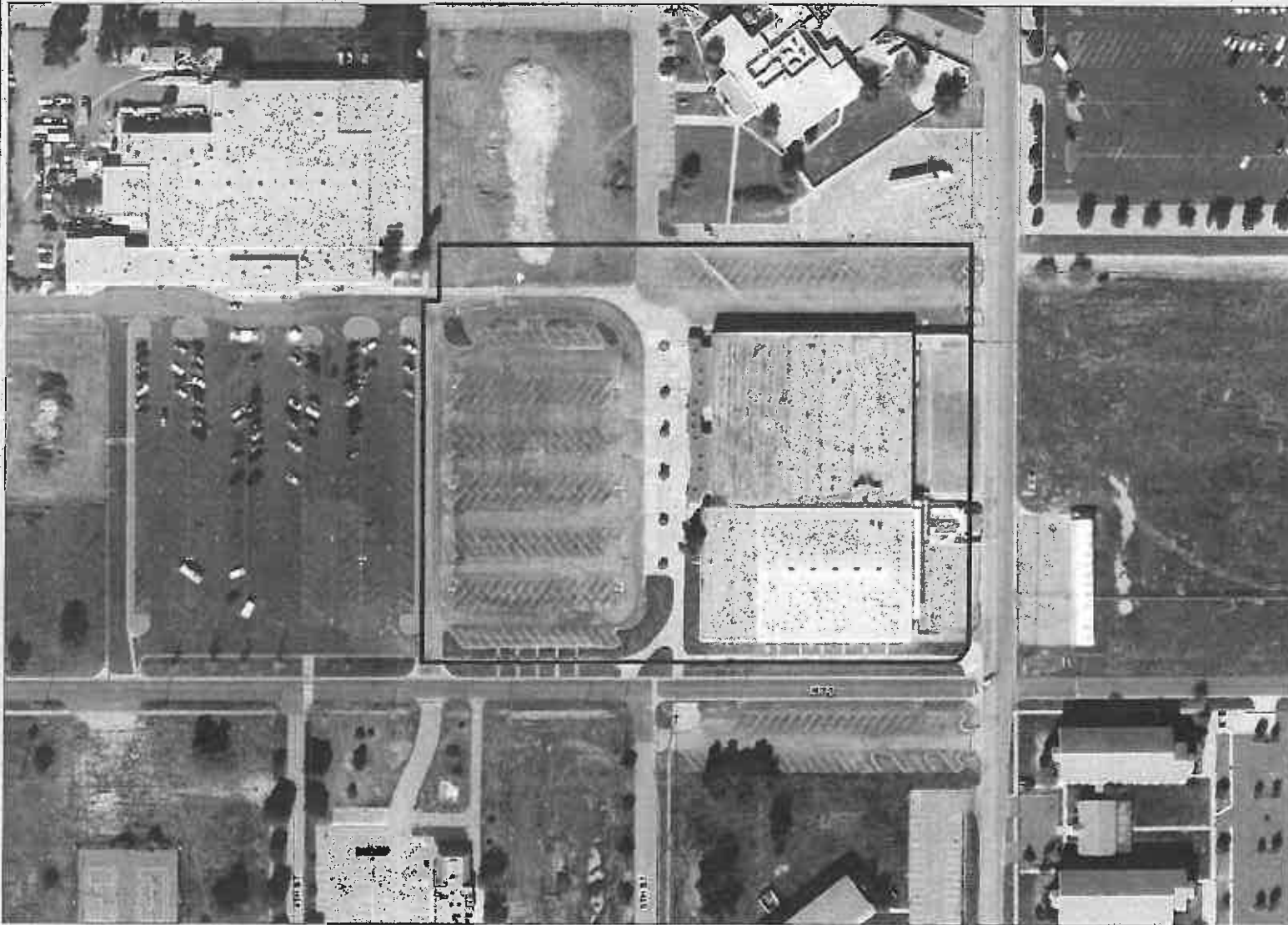
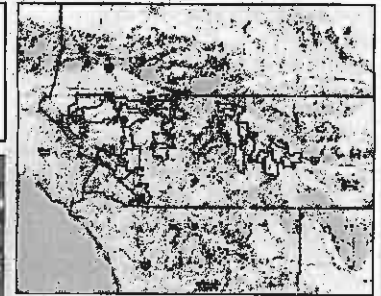
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/2/2016 9:01:42 AM

© Riverside County RCIT GIS

Notes

My Map



Legend

- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 214 427 Feet



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/2/2018 9:06:24 AM

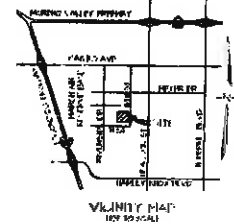
© Riverside County RCIT GIS

Notes

MARCH JPA - RIVERSIDE COUNTY, CALIFORNIA

MARCH LIFECARE CAMPUS - SIGNATURE HEALTH SITE

INSTITUTIONAL
RESIDENTIAL
PARCEL 25



PROJECT DESCRIPTION
A 100,000 SQ. FT. INSTITUTIONAL RESIDENTIAL CARE FACILITY ON 1.5 ACRES PARCEL INCLUDING PARKING, LANDSCAPE AND HIGH QUALITY FINISHES.

LEGAL DESCRIPTION
SOUTH PART OF LOT 4 AND 1/2 OF THE 1/4 SECTION 16, TOWNSHIP 11N, RANGE 12E, S. 1/4, R. 12E, S. 11N, COUNTY OF RIVERSIDE, CALIFORNIA, BEING PART OF THE MARCH AIR RESERVE BASE, RIVERSIDE COUNTY, CALIFORNIA, BEING MORE PARTICULARLY DESCRIBED AS SET FORTH IN THE RECORDS OF THE COUNTY CLERK OF RIVERSIDE COUNTY, CALIFORNIA, TO WIT:

SITE ADDRESS
1000 11TH STREET, RIVERSIDE, CALIFORNIA

- NOTES:**
1. THE SUBJECT PROPERTY IS A 1.5 ACRE PARCEL WITH A TOTAL AREA OF 65,340 SQ. FT. (1.5 ACRES).
 2. THE PROPERTY IS NOT SUBJECT TO ANY EASEMENTS OR ENCUMBRANCES OF ANY KIND.
 3. THE PROPERTY IS NOT SUBJECT TO ANY EASEMENTS OR ENCUMBRANCES OF ANY KIND.
 4. THE PROPERTY IS NOT SUBJECT TO ANY EASEMENTS OR ENCUMBRANCES OF ANY KIND.
 5. THE PROPERTY IS NOT SUBJECT TO ANY EASEMENTS OR ENCUMBRANCES OF ANY KIND.
 6. THE PROPERTY IS NOT SUBJECT TO ANY EASEMENTS OR ENCUMBRANCES OF ANY KIND.
 7. THE PROPERTY IS NOT SUBJECT TO ANY EASEMENTS OR ENCUMBRANCES OF ANY KIND.
 8. THE PROPERTY IS NOT SUBJECT TO ANY EASEMENTS OR ENCUMBRANCES OF ANY KIND.
 9. THE PROPERTY IS NOT SUBJECT TO ANY EASEMENTS OR ENCUMBRANCES OF ANY KIND.
 10. THE PROPERTY IS NOT SUBJECT TO ANY EASEMENTS OR ENCUMBRANCES OF ANY KIND.

SETBACKS:
FRONT: 10 FT.
REAR: 10 FT.
SIDE: 10 FT.

PERMITS REQUIRED:
GENERAL PERMIT
FIRE DEPARTMENT PERMIT
HEALTH DEPARTMENT PERMIT
PLANNING DEPARTMENT PERMIT
PUBLIC WORKS PERMIT
WATER QUALITY CONTROL PERMIT
SEWER PERMIT
CITY OF RIVERSIDE PERMIT

PERMITTED AREAS	
PROPOSED BUILDING	100,000 SQ. FT.
PROPOSED PARKING	100 SPACES
PROPOSED LANDSCAPE	10,000 SQ. FT.
PROPOSED WATER QUALITY CONTROL	10,000 GPD
PROPOSED SEWER	10,000 GPD
PROPOSED ELECTRICAL	10,000 VA
PROPOSED MECHANICAL	10,000 BTU
PROPOSED FIRE PROTECTION	10,000 SQ. FT.
PROPOSED SECURITY	10,000 SQ. FT.
PROPOSED ACCESSIBILITY	10,000 SQ. FT.
PROPOSED SIGNAGE	10,000 SQ. FT.
PROPOSED FENCE	10,000 SQ. FT.
PROPOSED LIGHTING	10,000 SQ. FT.
PROPOSED UTILITIES	10,000 SQ. FT.
PROPOSED OTHER	10,000 SQ. FT.

MARCH JOINT POWERS AUTHORITY
RIVERSIDE COUNTY, CALIFORNIA
SIGNATURE HEALTH SERVICE, LLC
MARCH LIFECARE CAMPUS
PLOT PLAN
PP-2024-001

DATE: 08/20/24
DRAWN: J. SMITH
CHECKED: M. JONES
SCALE: AS SHOWN
SHEET: 1 OF 1
PROJECT NO: 24-001

OWNER:
SIGNATURE HEALTH SERVICE, LLC
1000 11TH STREET
RIVERSIDE, CA 92504
TEL: (951) 509-1000
FAX: (951) 509-1001

APPLICANT:
SIGNATURE HEALTH SERVICE, LLC
1000 11TH STREET
RIVERSIDE, CA 92504
TEL: (951) 509-1000
FAX: (951) 509-1001

ENGINEER:
ALBERT A. HERRERA ARCHITECTS
1000 11TH STREET
RIVERSIDE, CA 92504
TEL: (951) 509-1000
FAX: (951) 509-1001

TOPOGRAPHY:
BLAND ASSOCIATES
1000 11TH STREET
RIVERSIDE, CA 92504
TEL: (951) 509-1000
FAX: (951) 509-1001

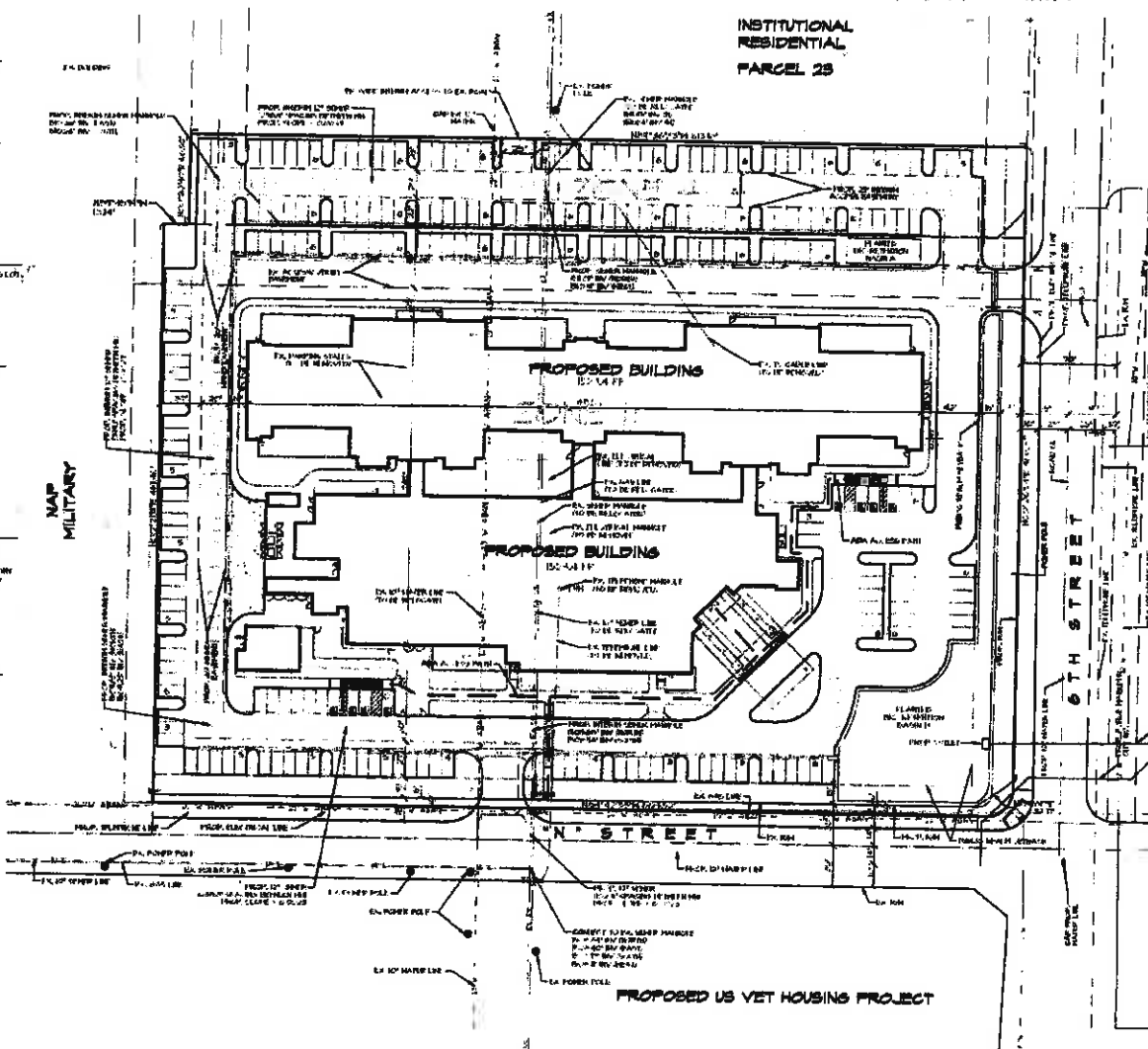
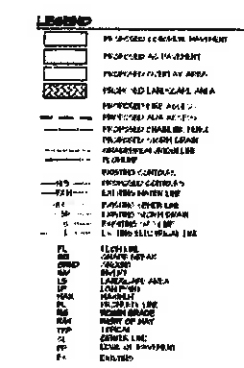
APN:
281-460-10

LAND USE / ZONING:
INSTITUTIONAL RESIDENTIAL
MARCH AIR RESERVE BASE
MARCH LIFECARE CAMPUS
MARCH LIFECARE CAMPUS

ACREAGE:
1.5 ACRES
100,000 SQ. FT.

EARTHWORK:
NO EARTHWORK

UTILITIES:
WATER: MARCH WATER DEPARTMENT
SEWER: MARCH SEWER DEPARTMENT
ELECTRIC: MARCH ELECTRIC DEPARTMENT
TELEPHONE: MARCH TELEPHONE DEPARTMENT
CABLE: MARCH CABLE DEPARTMENT
GAS: MARCH GAS DEPARTMENT



FUTURE REGIONAL
WATER QUALITY
DETENTION BASIN

PROPOSED US VET HOUSING PROJECT



INSTITUTIONAL
RESIDENTIAL
PARCEL 25

PROPOSED BUILDING

PROPOSED BUILDING

FUTURE REGIONAL
WATER QUALITY
RETENTION BASIN

MEYERS CHANNEL

WABOOK CHANNEL

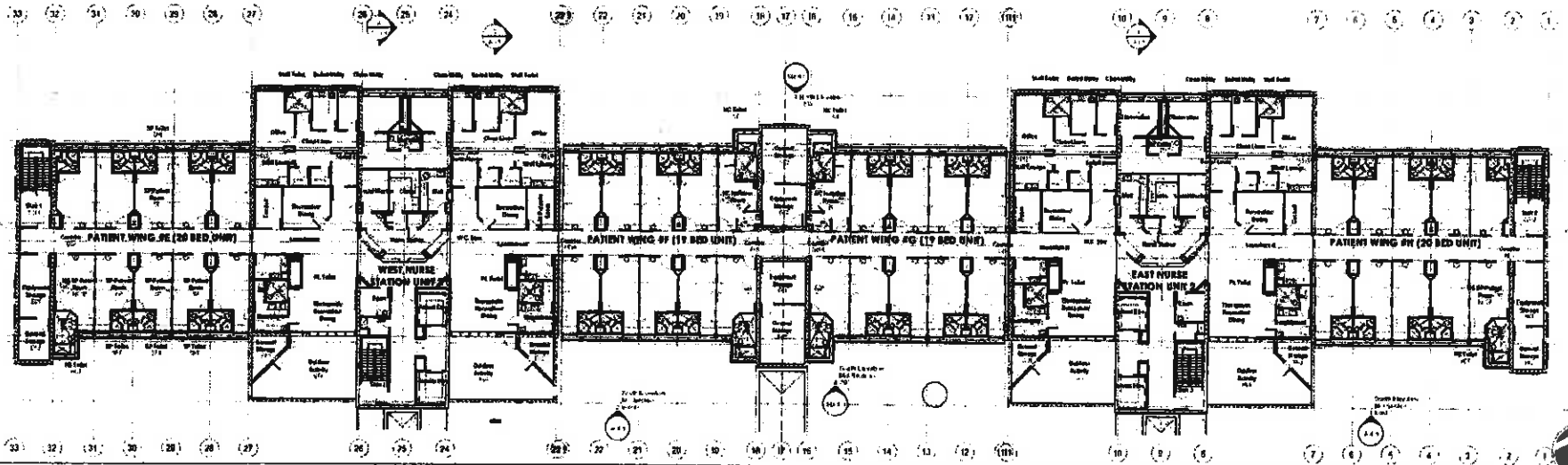
PROPOSED US VET HOUSING PROJECT



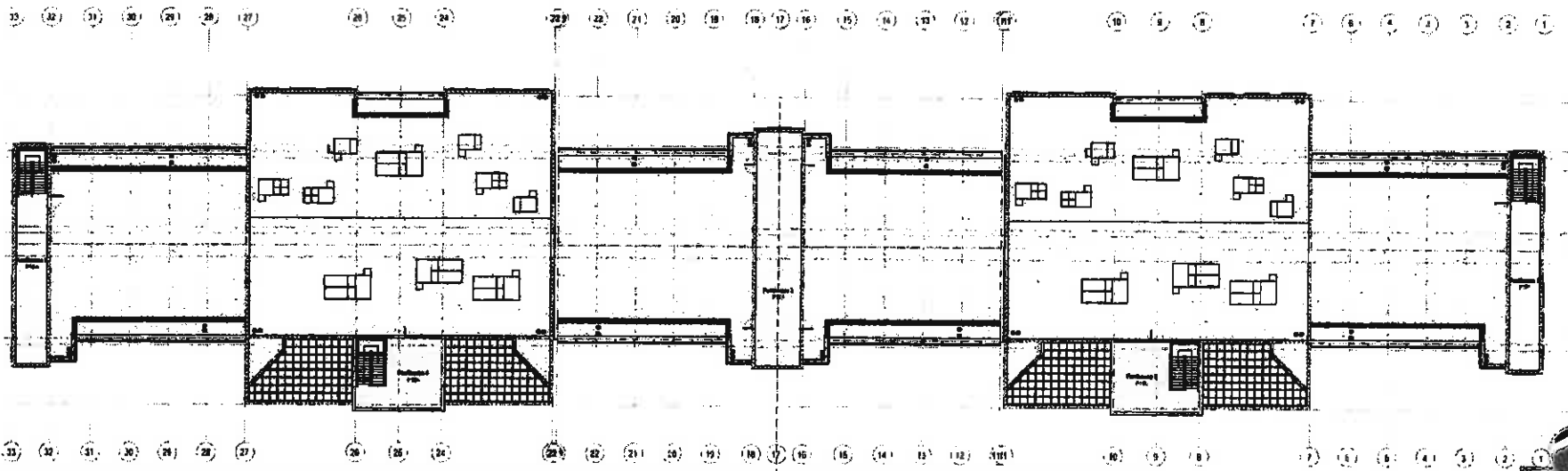
MARCH JOINT POWERS AUTHORITY
RIVERSIDE COUNTY, CALIFORNIA

SIGNATURE HEALTH SERVICE, LLC
MARCH LIFECARE CAMPUS
CONCEPTUAL GRADING PLAN

SCALE: 1"=20'	DATE: 05/20/14	DESIGNED: J. L. LEE	CHECKED: J. L. LEE	DATE: 05/20/14
PROJECT: 140		DRAWN: J. L. LEE		DATE: 05/20/14
PROJECT: 140		DRAWN: J. L. LEE		DATE: 05/20/14
PROJECT: 140		DRAWN: J. L. LEE		DATE: 05/20/14
PROJECT: 140		DRAWN: J. L. LEE		DATE: 05/20/14



Second Floor Plan - SD 1/16 - 1st 1



Penthouse Plan - SD 1/16 - 1st 2

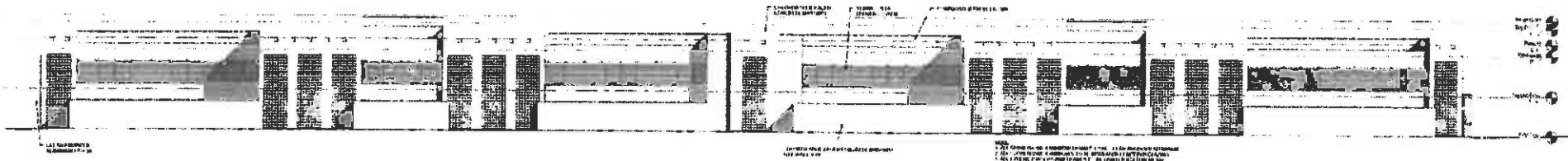
Riverside Behavioral Health Facility
Signature Healthcare Services, LLC

Second and Penthouse Floor Plans



GREGG MAEDO

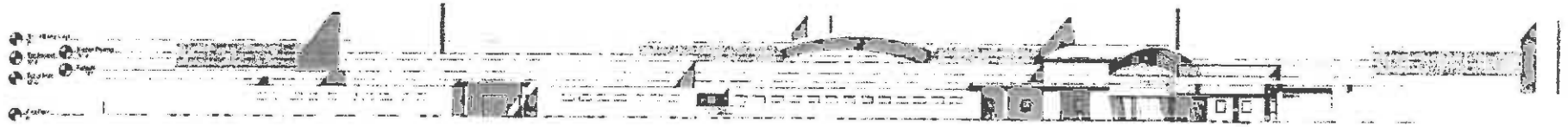
SD 3.1
ASSOCIATES, INC.
ARCHITECTURE - INTERIOR - PLANNING
121 NORTH PAMPANUE STREET, SUITE 101
ORANGE, CALIFORNIA 92668-1934
Tel: 949.474.7318



North Elevation - SD 1/16" = 1'-0" 1



North Elevation Mid Section - SD 1/16" = 1'-0" 2



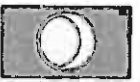
South Elevation - SD 1/16" = 1'-0" 3



South Elevation Mid Section - SD 1/16" = 1'-0" 4

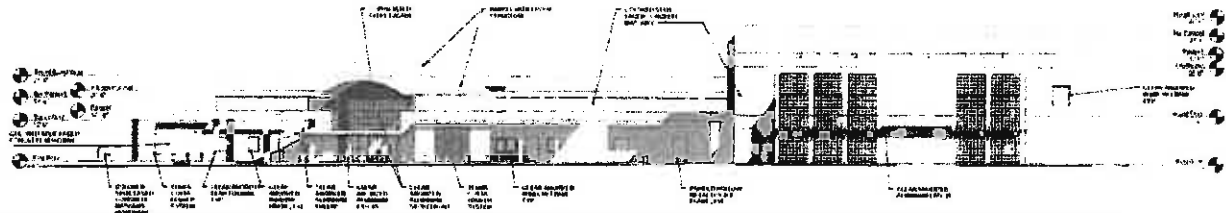
Riverside Behavioral Health Facility
Signature Healthcare Services, LLC

Exterior Elevations

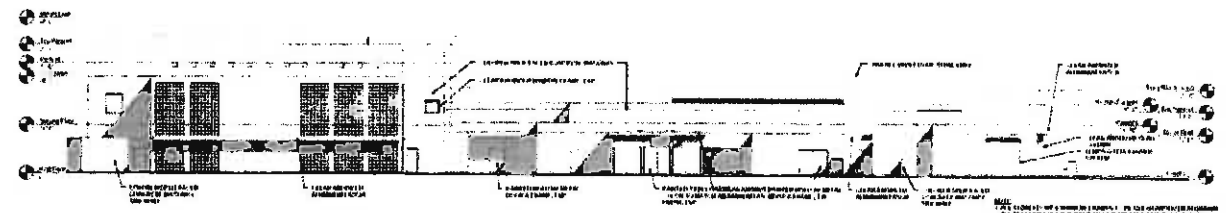


GREGG MAEDO

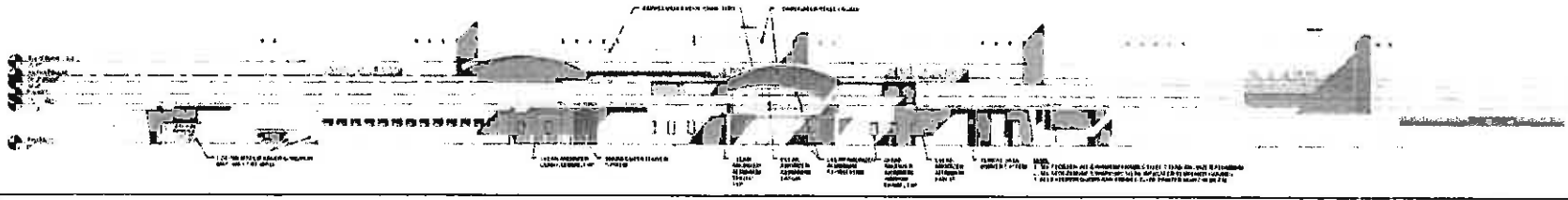
SD 4.0
P R S P O C I A T E S I N C.
ARCHITECTURE - INTERIORS - PLUMBING
121 INDIAN PAMPANORE STREET SUITE 101
ORANGE, CALIFORNIA 92668 P 714 791 1085
14895 FEBRUARY 4, 2014



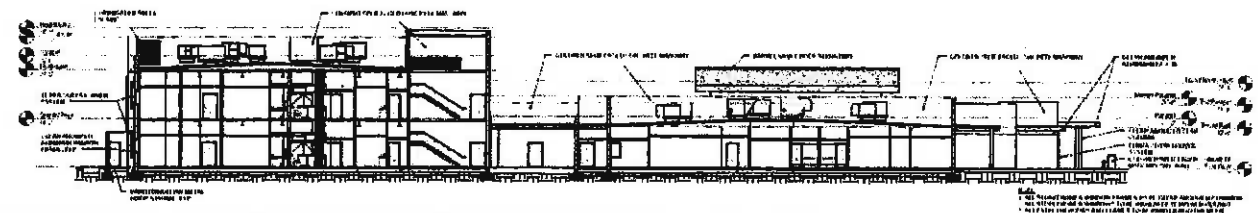
East Elevation - SD 1/16" = 1'-0" 1



West Elevation - SD 1/16" = 1'-0" 2



Entry Elevation - SD 1/16" = 1'-0" 3



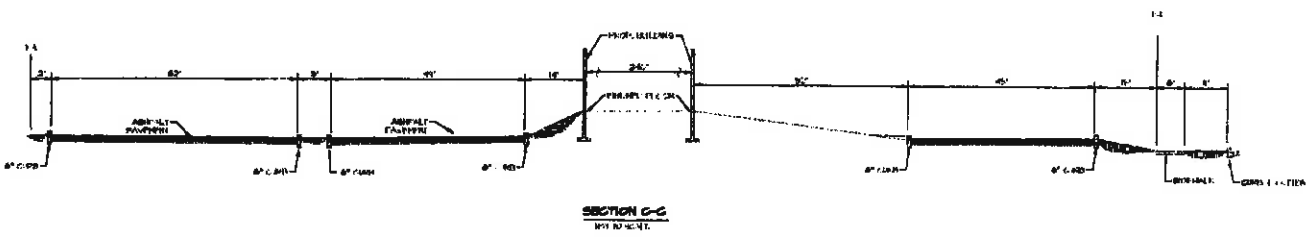
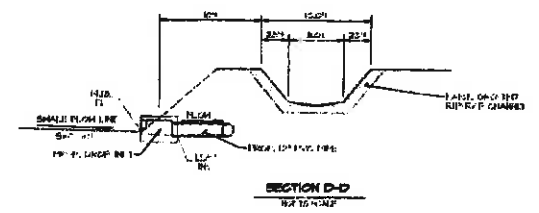
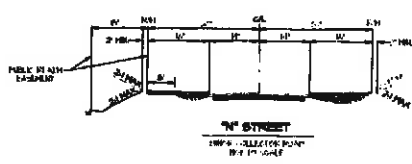
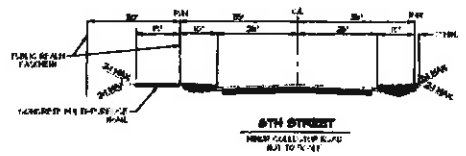
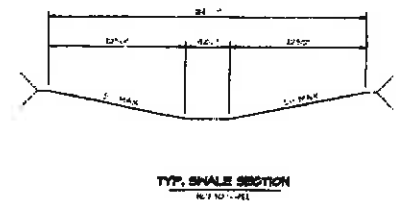
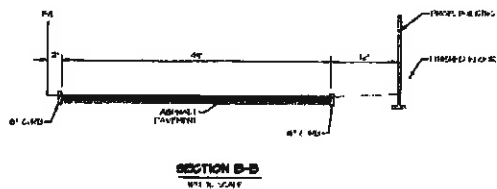
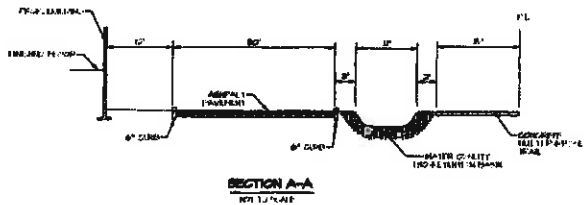
Section N-S Looking East - SD 1/16" = 1'-0" 4

Riverside Behavioral Health Facility
Signature Healthcare Services, LLC

Exterior Elevations and Section



GREGG MAEDO ASSOCIATES, INC.
ARCHITECTURE • INTERIORS • PLANNING
371 ROCKY RASHPARK STREET SUITE 101
CHAPARRAL, CALIFORNIA 91791
951.244.1111



MARCH JOINT POWERS AUTHORITY RIVERSIDE COUNTY, CALIFORNIA			
SIGNATURE HEALTH SERVICE, LLC MARCH LIFE CARE CAMPUS TYPICAL SECTIONS			
DATE: 2/20/20	DESIGNED BY: [Signature]	CHECKED BY: [Signature]	DRAWN BY: [Signature]
SCALE: AS SHOWN	PROJECT NO: 2019-001	SHEET NO: 3	TOTAL SHEETS: 3
PROJECT: MARCH LIFE CARE CAMPUS	LOCATION: MARCH	DATE: 2/20/20	SCALE: AS SHOWN

PAGE BREAK





AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

November 10, 2016

CHAIR
Simon Housman
Rancho Mirage

VICE CHAIRMAN
Rod Ballance
Riverside

COMMISSIONERS

Arthur Butler
Riverside

Glen Holmes
Hemet

John Lyon
Riverside

Steve Manos
Lake Elsinore

Russell Betts
Desert Hot Springs

STAFF

Director
Ed Cooper

John Guerin
Paul Rull
Barbara Santos

County Administrative Center
4080 Lemon St., 14th Floor
Riverside, CA 92501
(951) 955-5132

www.rcaluc.org

Mr. Sean Kelleher, Project Planner
City of Riverside Community and Economic Development Department/Planning Division
3900 Main Street, Third Floor
Riverside, CA 92522

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –
DIRECTOR’S DETERMINATION**

File No.: ZAP1225MA16
Related File No.: P16-0731 (Tentative Parcel Map), P16-0732 (Design Review)
APNs: 249-130-010, 249-130-052

Dear Mr. Kelleher:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed City of Riverside Case Nos. P16-0731 and P16-0732, a Design Review to construct five warehouse buildings ranging in size from 10,000 square feet to 13,850 square feet on 4.05 acres within two Assessor’s parcels located on the easterly side of Rustin Avenue, southerly of its intersection with Marlborough Avenue. The applicant is also proposing a Tentative Parcel Map to subdivide the 4.05-acre project site into five parcels.

The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, non-residential intensity is not restricted.

The elevation of the site is more than 500 feet lower than the elevation of March’s Runway 14-32 at its northerly terminus (approximately 1,535 feet above mean sea level), and the site is located more than 20,000 feet from runways at Riverside Municipal Airport and Flabob Airport. Therefore, FAA Obstruction Evaluation Service review for height/elevation reasons was not required.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, subject to the following conditions:

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.

AIRPORT LAND USE COMMISSION

2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris centers, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The landowner shall provide the attached notice to all potential purchasers of the property and to tenants/lessees of the building(s) thereon.

If you have any questions, please contact Paul Rull, ALUC Urban Regional Planner IV, at (951) 955-6893 or John Guerin, ALUC Principal Planner, at (951) 955-0982.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Edward C. Cooper, Director

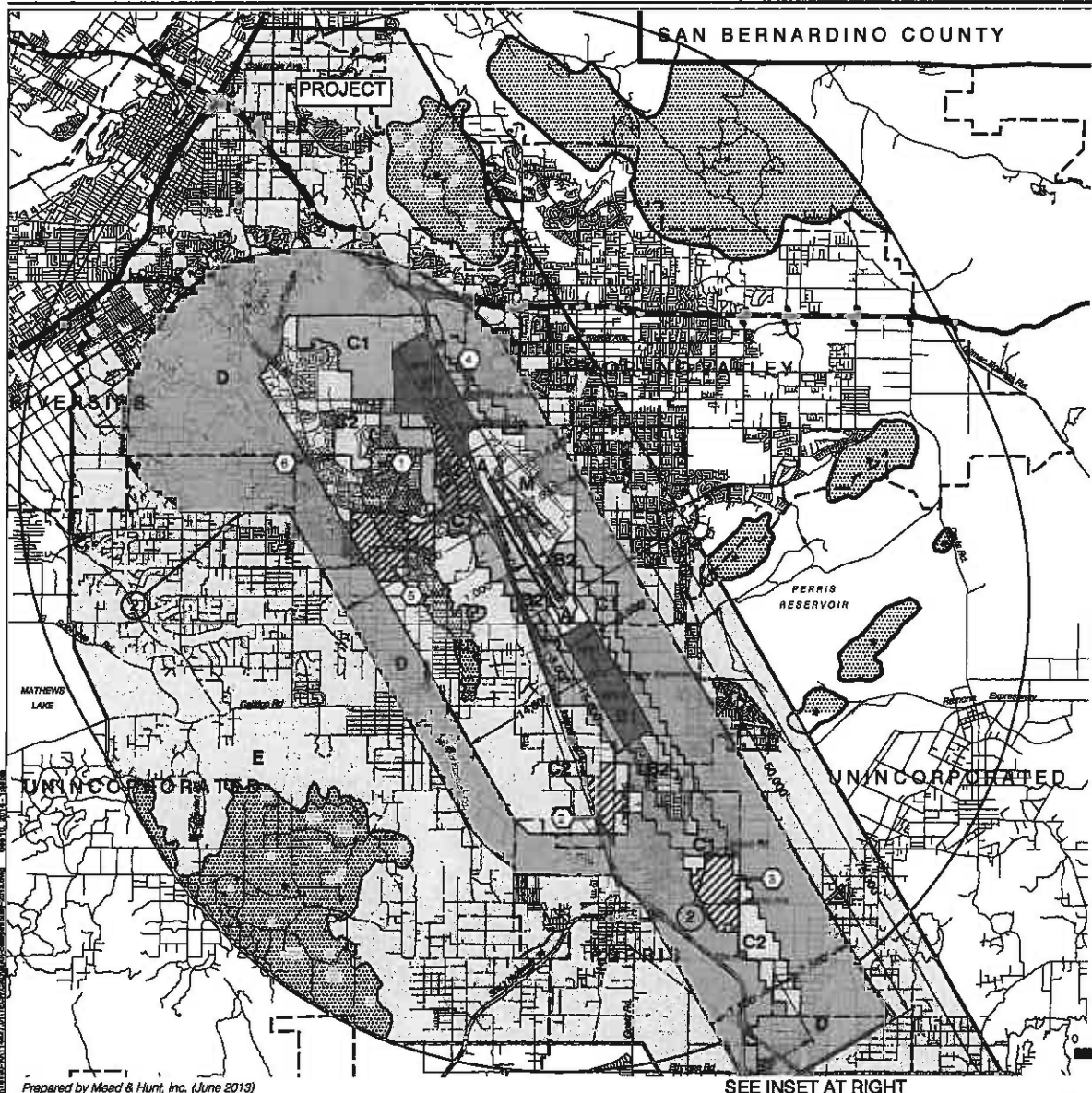
Attachments: Notice of Airport in Vicinity

cc: Rustin Business Park, LLC (applicant)
SDH & Associates (representative/payee)
Richard B. Lee (listed property owner)
Gary Gosliga, Airport Manager, March Inland Port Airport Authority
Denise Hauser, March Air Reserve Base
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1225MA16\ZAP1225MA16.LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



LEGEND

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

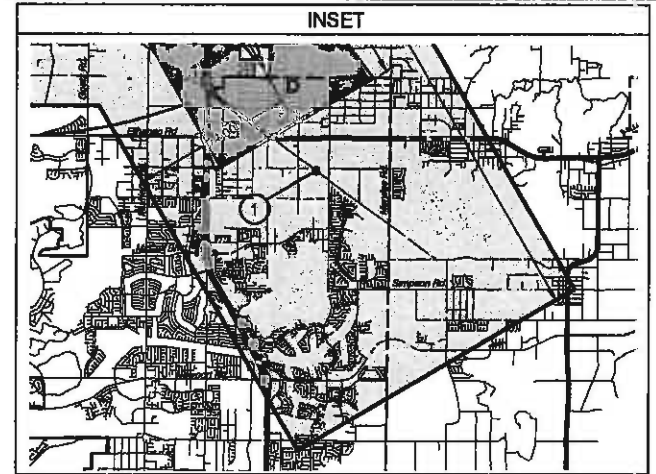
Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- ▨ Site-Specific Exceptions (existing local agency commitments to development projects)

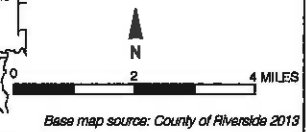
① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.

② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



Note:
All dimensions are measured from runway ends and centerlines.



**Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)**

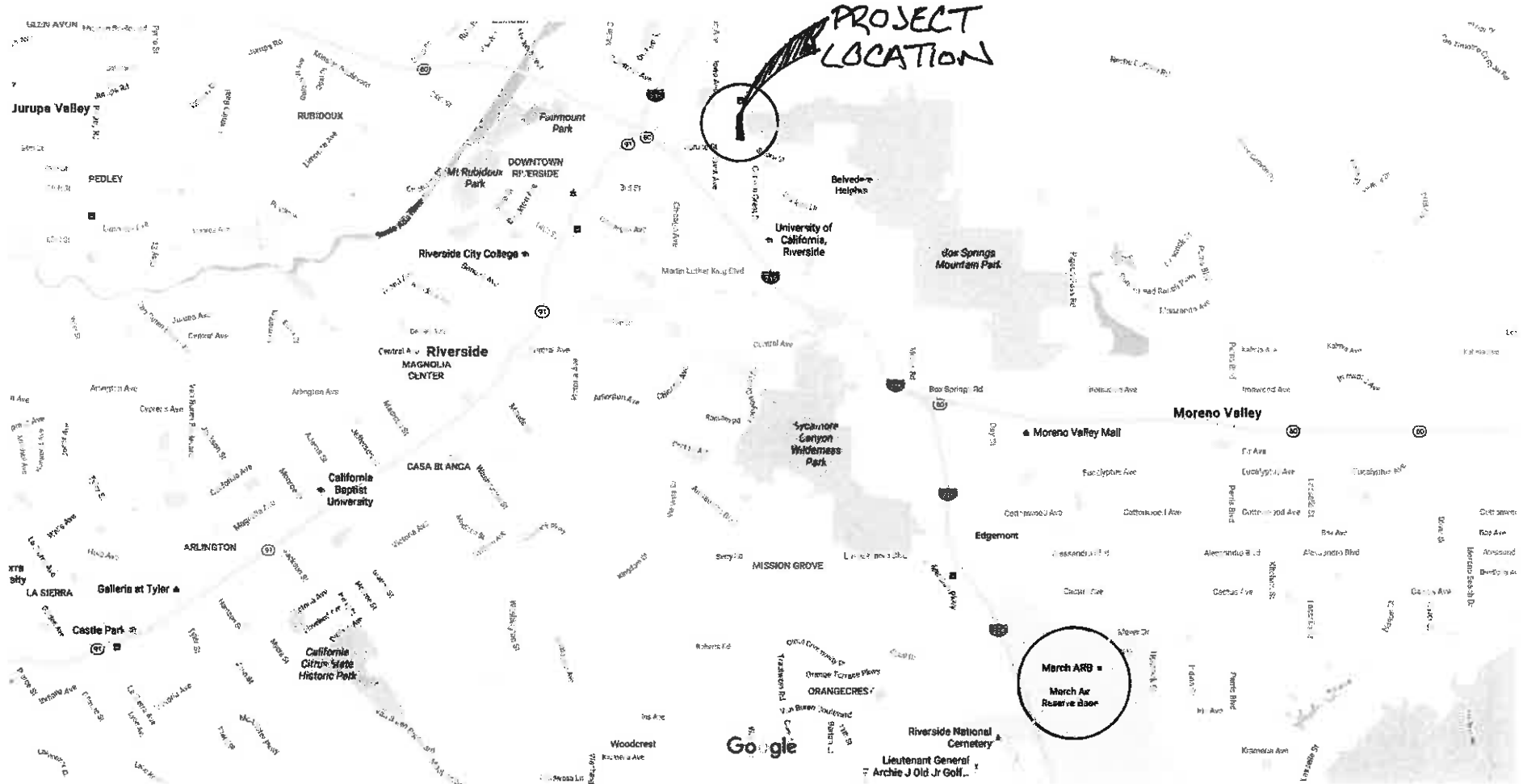
Map MA-1

Compatibility Map
March Air Reserve Base / Inland Port Airport

Prepared by Mead & Hunt, Inc. (June 2013)

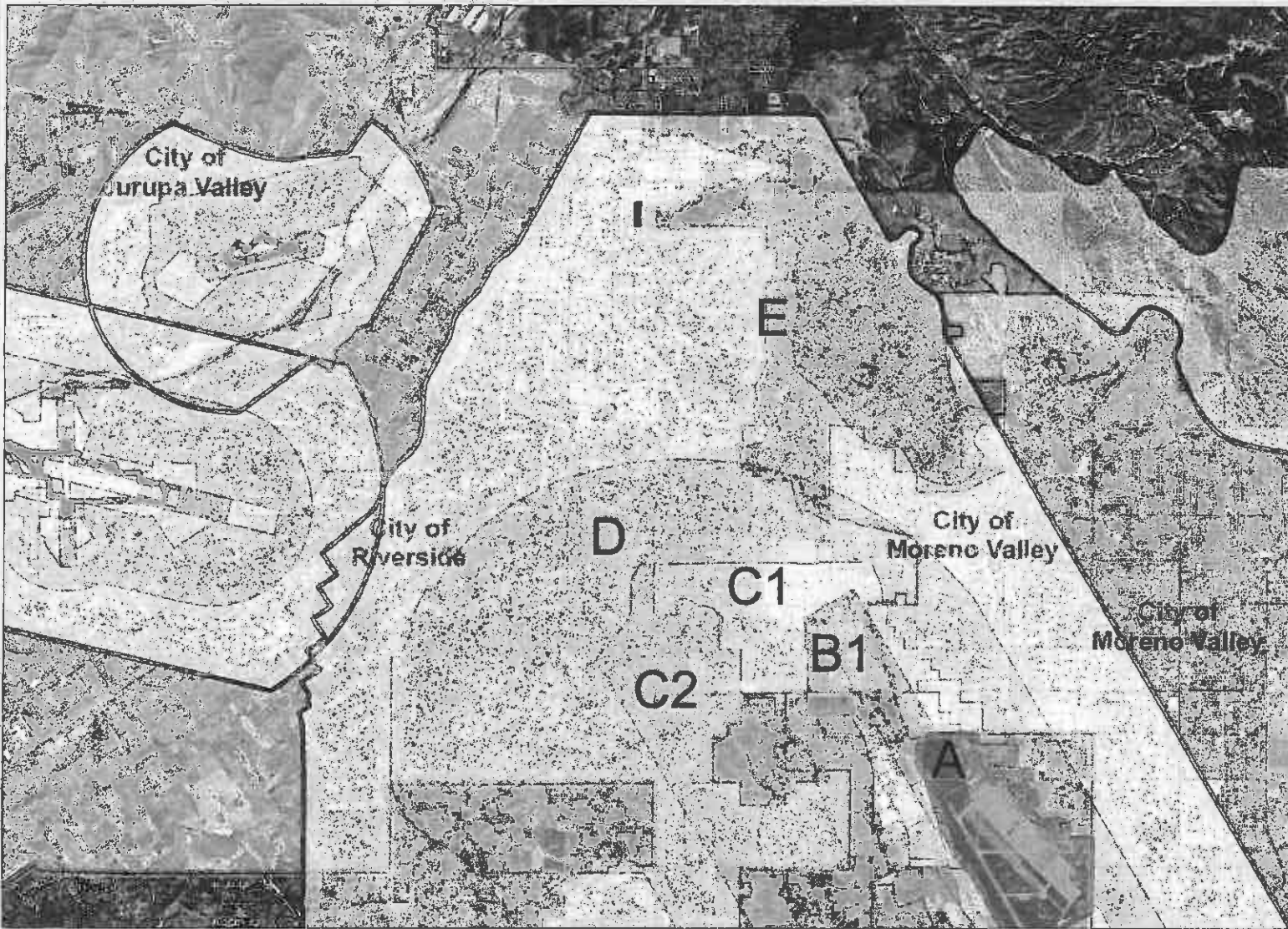
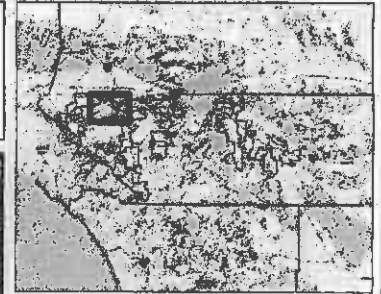
SEE INSET AT RIGHT

Google Maps



Map data ©2016 Google 1 mi

My Map



Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



0 11,244 22,488 Feet



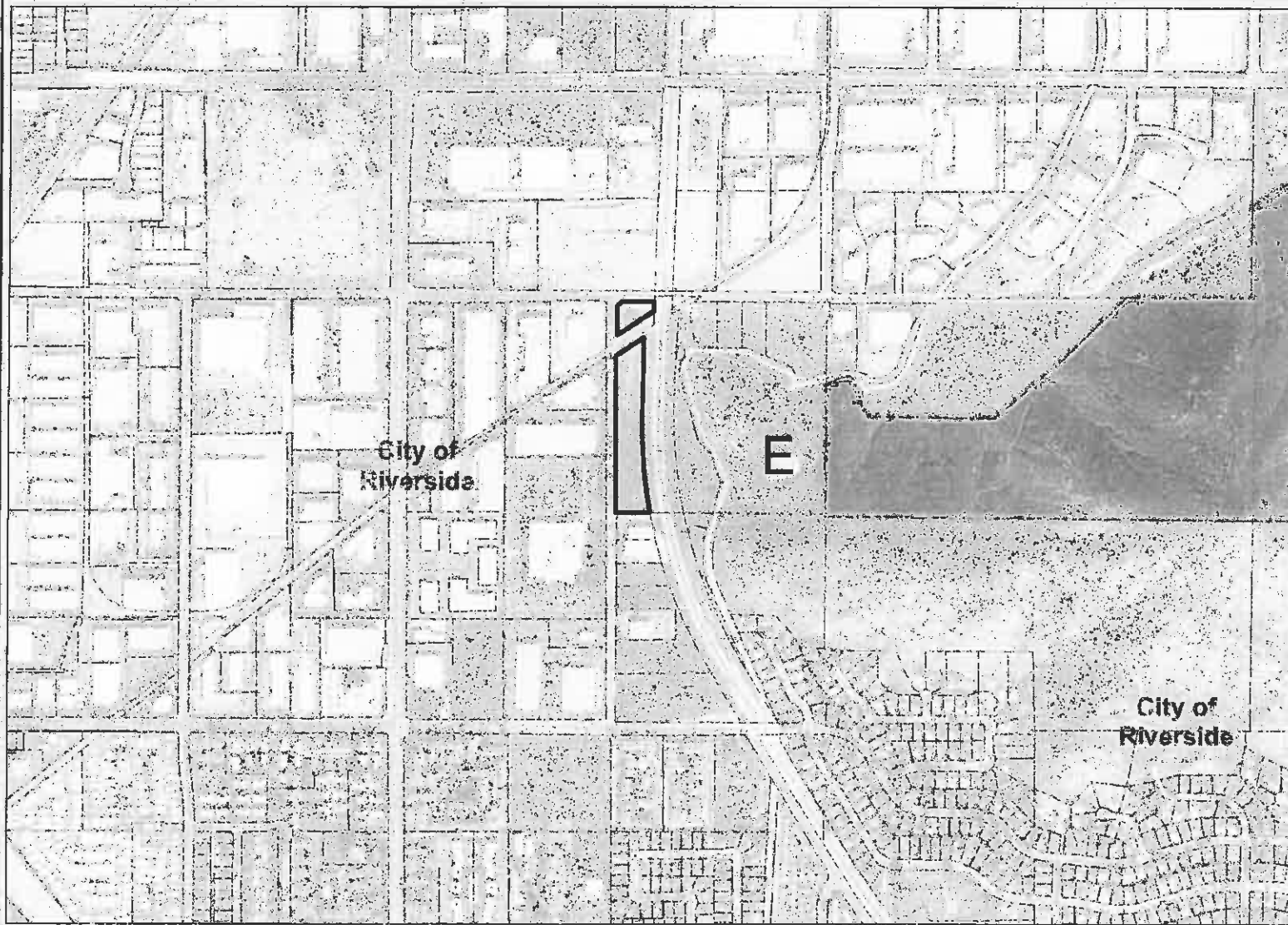
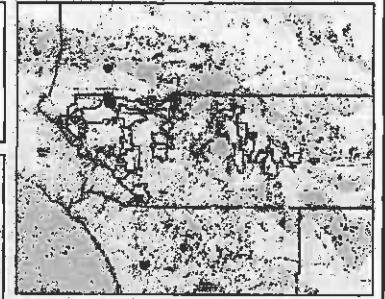
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/1/2016 12:56:43 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC8



0 1,148 2,296 Feet



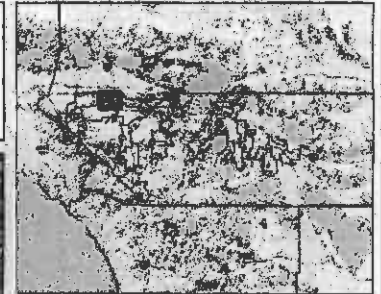
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/1/2016 12:54:54 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- City Boundaries
- Cities**
- highways_large**
- HWY
- INTERCHANGE
- INTERSTATE
- USHWY
- majorroads
- counties
- cities
- hydrographylines
- waterbodies**
- Lakes
- Rivers



0 5,622 11,244 Feet



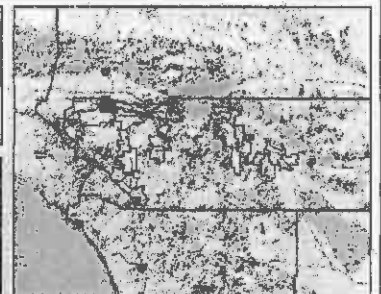
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/1/2016 12:57:45 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- City Boundaries
- Cities**
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads**
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities
- hydrographylines
- waterbodies**
- Lakes
- Rivers



0 2,811 5,622 Feet



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/1/2016 12:58:22 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0

1,405

2,811 Feet



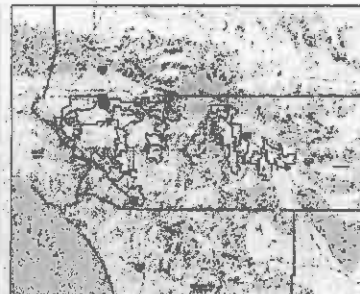
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/1/2016 12:58:50 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities**
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

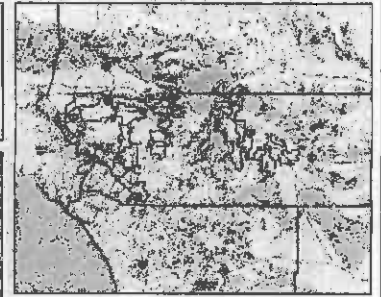


REPORT PRINTED ON... 11/1/2016 12:55:35 PM













© Riverside County RCIT GIS

Notes

My Map



Legend

-  Display Parcels
-  City Boundaries
- Cities**
- roadsanno
- highways
 -  HWY
 -  INTERCHANGE
 -  INTERSTATE
 -  OFFRAMP
 -  ONRAMP
 -  USHWY
-  counties
-  cities
- hydrographylines
- waterbodies
 -  Lakes
 -  Rivers



0 703 1,405 Feet



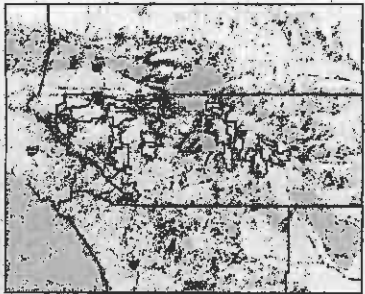
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/1/2016 12:59:23 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 351 703 Feet



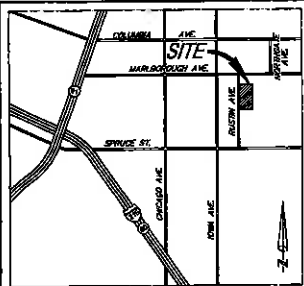
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/1/2016 12:59:50 PM

© Riverside County RCIT GIS

Notes

IN THE CITY RIVERSIDE, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA
TENTATIVE PARCEL MAP 37238
 OCTOBER 2016
 SEC 18 T2S, R4W, SB8M



VICINITY MAP
 NOT TO SCALE
 THOMAS GUIDE PG. 714, PORT. GRID H-3 & H-4

OWNER/APPLICANT
 RUSTIN BUSINESS PARK LLC
 ATTN: ARL MOORE CLARK
 101 HEDGECOCK RD
 IRVINGSHAW, CA 91380
 TEL: (805) 494-7704

ENGINEER
 SOH & ASSOCIATES, INC
 2225 CHERRY CREST DRIVE 71439
 RIVERSIDE, CA 92507
 VOICE: (951) 523-2001
 FAX: (951) 708-5174

SOILS ENGINEER
 GEOMAT TESTING LABORATORIES, INC
 9105 BURNER AVE., STE. 14
 RIVERSIDE, CA 92503
 VOICE: (951) 688-5400

ASSESSOR'S PARCEL NO.
 248-130-010

ZONING AND LAND USE

EXISTING ZONING - BMP
 PROPOSED ZONING - BMP

SURROUNDING LAND USE

NORTH.....INDUSTRIAL
 EAST.....RIE
 SOUTH.....INDUSTRIAL
 WEST.....INDUSTRIAL/OFFICE

SURROUNDING ZONING

NORTH.....BMP
 EAST.....BMP
 SOUTH.....BMP
 WEST.....BMP

SCHOOL DISTRICT

RIVERSIDE UNIFIED
 SCHOOL DISTRICT

PROJECT INFO

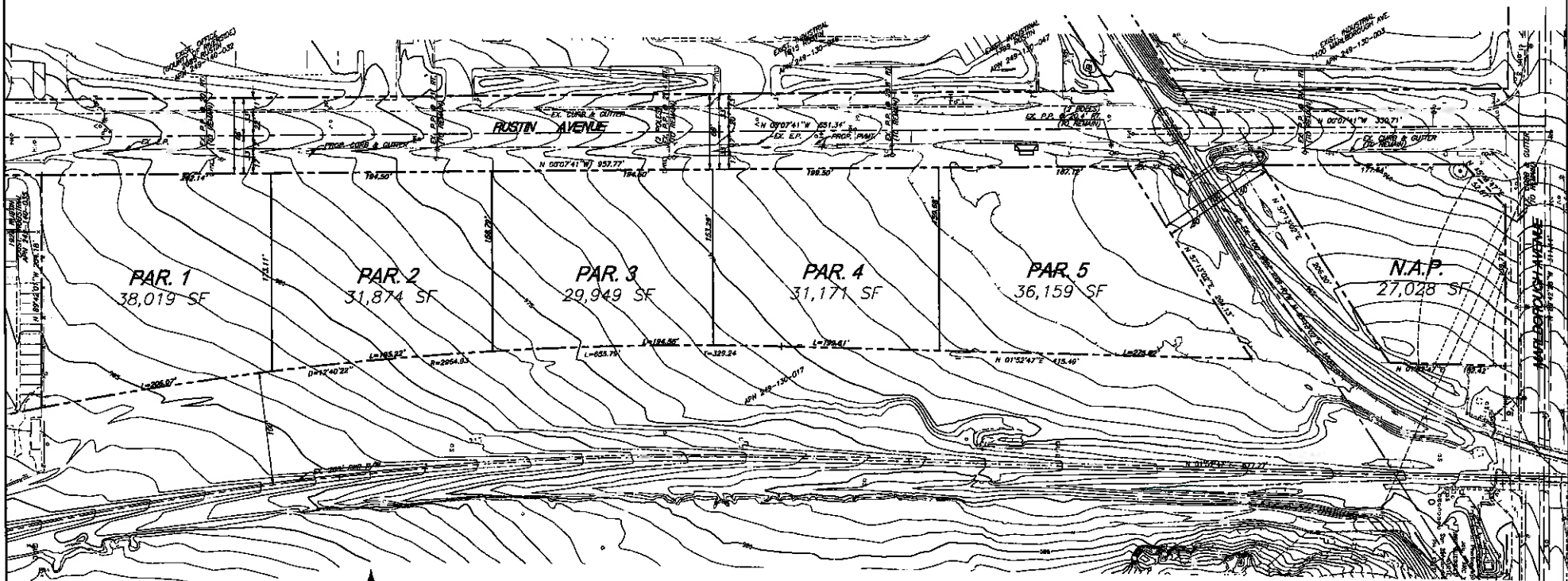
AREA OF SITE = 176,418 SF (4.05 AC)

UTILITY PURVEYORS

WATER.....CITY OF RIVERSIDE
 GAS.....SCL CALIF. GAS
 ELECTRICAL.....CITY OF RIVERSIDE
 TELEPHONE.....PACIFIC BELL
 SATELITE.....CITY OF RIVERSIDE
 CABLE.....CHARTER COMMUNICATION

LEGEND

- P.P. POWER POLE
- EXST. EXISTING
- TRACT BOUNDARY
- CEMENTLINE
- CURB AND GUTTER
- LOT LINE



PAR. 1
 38,019 SF

PAR. 2
 31,874 SF

PAR. 3
 29,949 SF

PAR. 4
 31,171 SF

PAR. 5
 36,159 SF

N.A.P.
 27,028 SF



REVISIONS

NO.	DESCRIPTION	BY	DATE

PLANNING DIVISION

DATE: _____

PREPARED BY:

DATE: _____

ROBERT VANZANTEN
 R.C.E. NO.: 82322

SEAL



PREPARED BY:

DATE: _____

SCALE

1"=40'

DATE: OCTOBER 2016



SOH AND ASSOCIATES INC.
 2225 CHERRY CREST DRIVE 71439
 RIVERSIDE, CALIFORNIA 92507
 TEL: (951) 523-2001 FAX: (951) 708-5174

BENCHMARK

POINT 25 36-03
 1988 BENCHMARK 433.375
 ADJUSTMENT YEAR 1983

FOR AFE AND CITY ENGINEER AND IN THE SOUTHWEST CORNER OF A
 CURB (FROM ALONG THE ELY CURB OF CON. INC. AT THE SE
 CURB RETURN OF SOH AVE. AND MARLBOROUGH AVE.

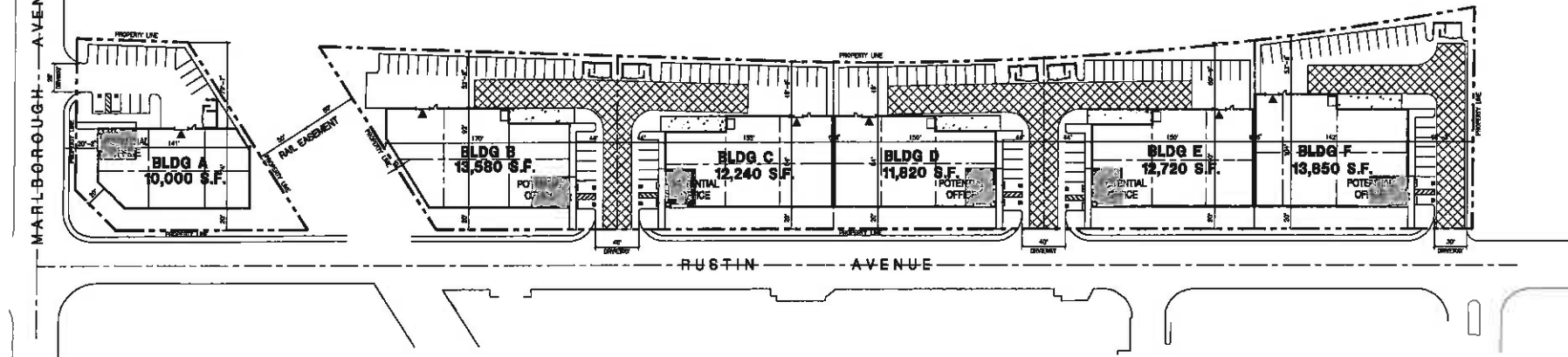
BUSINESS DIV. CERTIFICATE NO. 107294 EXP. 8-13-17

CITY OF RIVERSIDE
RUSTIN BUSINESS PARK, LLC
TENTATIVE PARCEL MAP NO. 37238

1
 OF
 1
 SHEETS

A.T. AND S.F. RAILROAD

MARLBOROUGH AVENUE



MASTER SITE PLAN



Utilities
WALTON CITY OF RIVERSIDE WATER DEPARTMENT 1000 W. MAIN ST. RIVERSIDE, CA 92501 (951) 763-5150
CABLE TV CHARTER COMMUNICATIONS 1327 CENTRAL AVE. RIVERSIDE, CA 92504 (951) 525-5111
GAS SOUTHERN CALIFORNIA GAS CO. 16231 VALLEY BLVD. FORTWORTH, TX 76126 (800) 427-2302
ELECTRICITY TO CAL PEBBON ELECTRIC 1600 S. MAIN ST. RIVERSIDE, CA 92501 (951) 246-3340
MUNDO CHICAGO, CA 91729 (951) 246-3340
3237 CENTRAL AVE. RIVERSIDE, CA 92504 (951) 494-2427
PHONE 951 2502 7115P ST. RIVERSIDE, CA 92503 (951) 383-0080

Project Information/Building Data
Zoning - BUSINESS & MANUFACTURING PERK (BMP)
CONSTRUCTION TYPE
OCCUPANCY
5 1/2 STORY
GENERAL PLAN
AUTOMATIC FIRE SPRINKLER
PLANNING DEPT CASE #

Property owner M5 RUSTIN LLC
107 HOOKSHAW ROAD, SUITE 200
THOUSAND OAKS, CA 91320
PHONE: (805) 494-1206
FAX: (805) 494-1209
CONTACT: MORGAN BRAD

Applicant M5 RUSTIN LLC
107 HOOKSHAW ROAD, SUITE 200
THOUSAND OAKS, CA 91320
PHONE: (805) 494-1206
FAX: (805) 494-1209
CONTACT: MORGAN BRAD

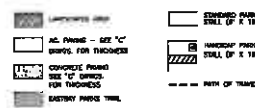
Applicant's representative
TERRY WEE
16331 BARNEYS AVE. STE J 100
TUSTIN, CA 92680
TEL: 949-261-2157
FAX: 949-261-0251
4276 ALAN SANDOVAL

Address of the property
SOUTHWEST CORNER OF MARLBOROUGH AVE AND RUSTIN AVE.
Assessor's Parcel Number
249-070-011

SITE PLAN GENERAL NOTES

1. THE SOILS REPORT PREPARED BY _____
2. IF SOILS ARE DETERMINED TO REQUIRE USE STEEL REINFORCING FOR ALL SITE CONCRETE.
3. ALL CONCRETE ARE TO THE FACE OF CONCRETE WALL, FACE OF CONCRETE CURB OR ONE USE CURB.
4. SEE "C" PLANS FOR ALL CONCRETE CURBS, GUTTERS AND CHANNELS. DETAILS ON SHEET 401 ARE SHOWN FOR REFERENCE.
5. THE ENTIRE PROJECT SHALL BE PERMANENTLY IMPROVED WITH AN AUTOMATIC FIRE SPRINKLER SYSTEM TO BE INSTALLED AT A MINIMUM TO MEET THE CALIFORNIA FIRE CODE AS REQUIRED.
6. SEE "C" DRAWINGS FOR PLAN OF CONCRETE OR GUTTER-SEE DRAWINGS. CONTRACTOR SHALL VERIFY ACTUAL UTILITY CONTRACTOR SHALL VERIFY ACTUAL UTILITY LOCATIONS.
7. PROVIDE POSITIVE DRAINAGE AWAY FROM BLDG. SEE "C" DRAWINGS.
8. CONTRACTOR TO REFER TO "C" DRAWINGS FOR ALL UNDERGROUND UTILITIES. SEE PLANS FOR GUIDANCE AND SPACING LOCATIONS.
9. SEE "C" DRAWINGS FOR FRESH DRINK ELEVATIONS.
10. CONCRETE SHOULD BE A MINIMUM OF 4" THICK BY TYPICAL JOINTS AT 8' O.C. EDWARDS/CONSTRUCTION JOINTS SHALL BE A MINIMUM OF 6" O.C. BY 1/2" BARS. SEE "C" DRAWINGS FOR MORE COMPREHENSIVE INFORMATION. SEE "C" DRAWINGS FOR DETAILS.
11. TREE CUTS ARE TO BE MADE TO REMAIN TO REMAIN OF TREE. LINES AS REQUIRED BY THE CONTRACTOR.
12. UNDERGROUND UTILITY SHALL BE DELIMITED WITH A MINIMUM 2' BUFFER OF HIGH CURB.
13. APPROVED CONCEPTUAL LANDSCAPE PLAN PRIOR TO GRADING POINT.

SITE LEGEND



TABULATION

SITE AREA	BLDG A	BLDG B	BLDG C	BLDG D	BLDG E	BLDG F	TOTAL
In s.f.	27,027	36,158	31,171	28,848	31,874	38,019	194,189 s.f.
In acres	0.62	0.83	0.72	0.66	0.73	0.87	4.48 ac
BUILDING AREA							
Office	1,500	1,500	1,500	1,500	1,500	1,500	9,000 s.f.
Warehouse	8,500	12,080	10,740	10,320	11,220	12,350	85,210 s.f.
TOTAL	10,000	13,580	12,240	11,820	12,720	13,850	74,210 s.f.
COVERAGE	37.0%	37.6%	38.3%	38.5%	38.6%	38.4%	38.2%
AUTO PARKING REQUIRED							
Office	1/250 s.f.	6	6	6	6	6	36 stalls
Warehouse	1/1,000 s.f.	9	12	11	10	11	65 stalls
TOTAL	15	18	17	16	17	18	101 stalls
AUTO PARKING PROVIDED							
Standard (8' x 15')	13	23	18	18	21	19	113 stalls
Handicap	2	2	2	2	2	2	12 stalls
TOTAL	15	25	20	21	23	21	125 stalls
ZONING ORDINANCE FOR CITY							
Zoning Designation - Business and Manufacturing perk (BMP)							
MAXIMUM BUILDING HEIGHT ALLOWED							
Height - 45', variance - 1', Actual - 55' max.							
MAXIMUM FLOOR AREA RATIO							
FAR - 1.50							
LANDSCAPE REQUIREMENT							
Percentage - to be verified							
SETBACKS							
Building							
Front - 40' (Bldg. over 30' H), 20' (Bldg. 30' or less in H.)							
Side/Rear - none							
Landscape							
10' (20 or fewer parking spaces)							
15' (21 or greater parking spaces)							

VICINITY MAP





HPA, Inc.
19813 Dardow Avenue - 4th
Floor, Irvine, CA
92612
Tel: 949-453-1770
Fax: 949-453-2851
email: hpa@hpaarch.com

Owner:

Project:

Consultants:

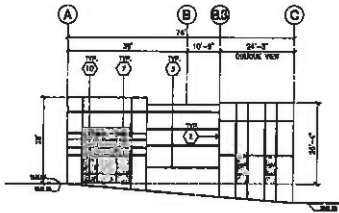
- CK - Structural
- Structural
- Mechanical
- Plumbing
- Electrical
- Landscape
- Fire Protection
- Soft Engineer

Title: Revision:

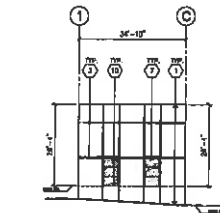
Project Number:
Drawn by:
Date:
Revised:

Sheet:

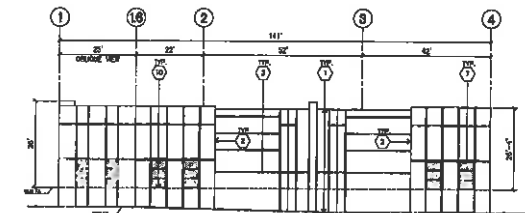
A3.1



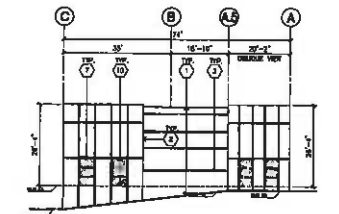
NORTH ELEVATION
scale 1/8" = 1'-0"



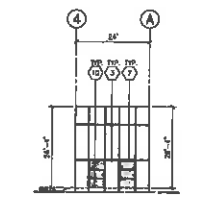
NORTHWEST ELEVATION
scale 1/8" = 1'-0"



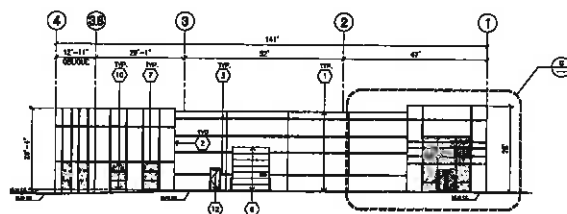
WEST ELEVATION
scale 1/8" = 1'-0"



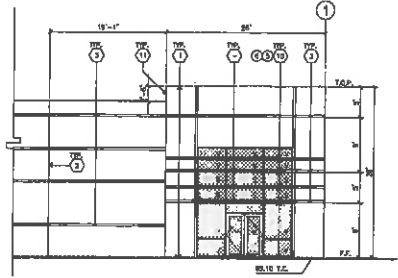
SOUTH ELEVATION
scale 1/8" = 1'-0"



SOUTHEAST ELEVATION
scale 1/8" = 1'-0"



EAST ELEVATION
scale 1/8" = 1'-0"



ENLARGED EAST ELEVATION
scale 1/8" = 1'-0"

ELEVATION KEYNOTES

- ① CONCRETE TILT-UP PANEL (PAINTED)
- ② PANEL JOINT
- ③ PANEL SEAM
- ④ METAL LOUVER
- ⑤ 3'-0" W X 10'-0" OVERHEAD DOOR @ DOOR HEAD
- ⑥ 12" W X 16" H OVERHEAD DOOR @ DRIVE THRU
- ⑦ METALLIC STEEL ALUMINUM FINISHED METAL PANEL SYSTEM
- ⑧ CONCRETE STAIR LANDING AND CONC. STAIRWELL
- ⑨ DOOR SWAPPER
- ⑩ ALUMINUM STOREFRONT FRAMING W/ TYPED GLAZING AT ALL DOORS. RECESSES ADJACENT TO DOORS AND GLAZING W/ BOTTLES LESS THAN 16" ABOVE F.F. ELEVATION.
- ⑪ FINISH SECTION
- ⑫ HOLLOW METAL DOOR
- ⑬ EXTERIOR CORNERPOST AND 2 OVERFLOW SCUPPERS
- ⑭ INTERIOR CORNERPOST AND 2 OVERFLOW SCUPPERS

GLAZING LEGEND

- WINDOW GLAZING
- SPHINDLE GLAZING

ALL GLAZING TO BE TYPED
SLIPING PER INTERNAL CG PROFILES & SQUARE OR GO SLIP
IN GLAZING SEALS. SEE SPEC.
1" INSULATED GLAZING UNIT WITH 1/2" AIRSPACE AND 1/2" TYP. UNITS
MULLIONS FINISHED SLIP

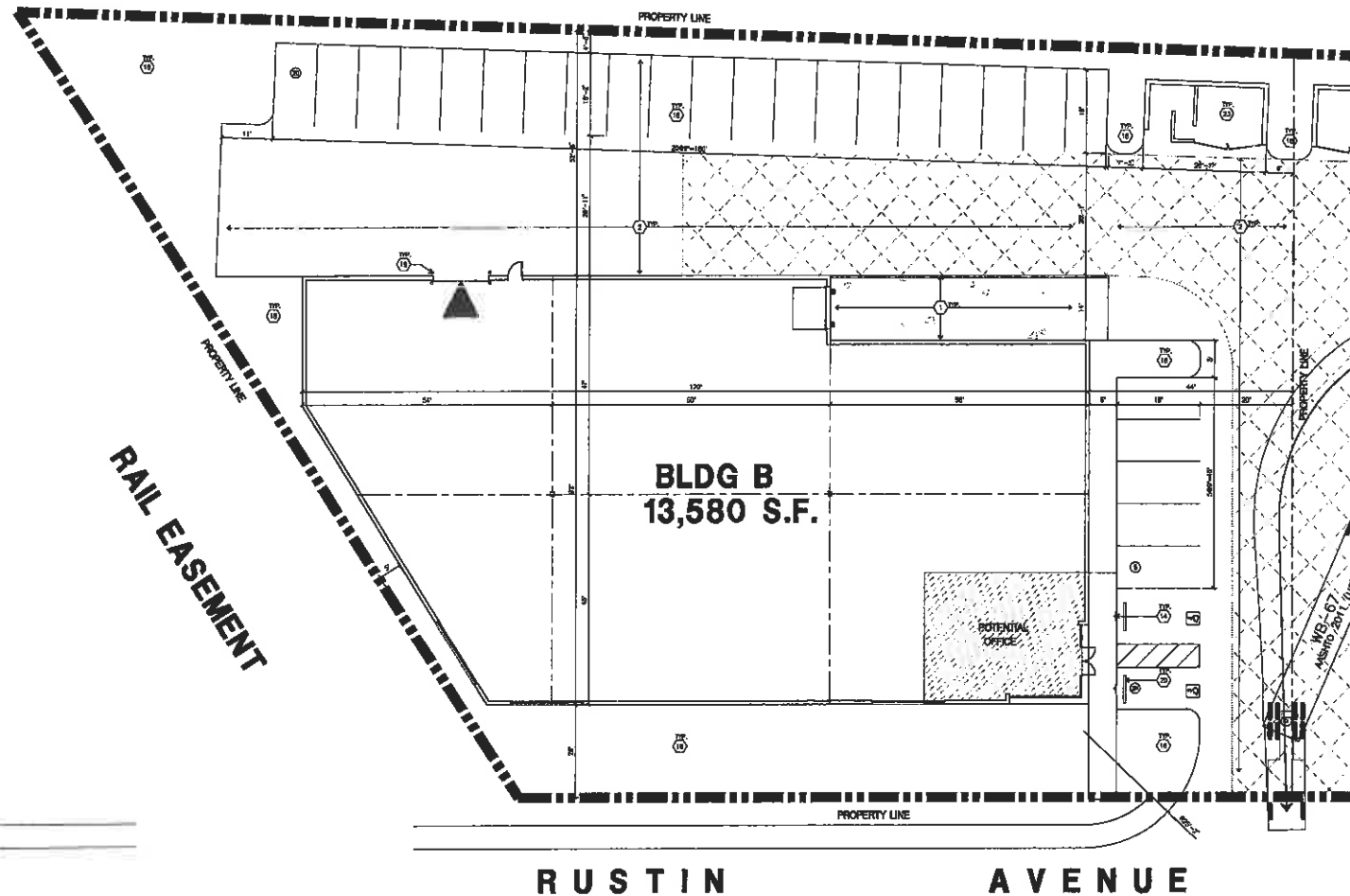
ELEVATION GENERAL NOTES

- A. ALL PAINT COLORS CHANGED TO OCCUR AT INSIDE CORNERS UNLESS NOTED OTHERWISE.
- B. ALL PAINT FINISHES ARE TO BE FLAT UNLESS NOTED OTHERWISE.
- C. T.O.P. = TOP OF FINISH FLOOR ELEVATION.
- D. F.F. = FINISH FLOOR ELEVATION.
- E. STOREFRONT CONSTRUCTION: GLASS, METAL ATTACHMENTS AND LANTIS TO BE DESIGNED TO RESIST 80 MPH EXPOSURE TO WIND. CONTRACTOR TO SUBMIT SHOP DRAWINGS PRIOR TO INSTALLATION.
- F. CONTRACTOR SHALL FULLY PAINT ONE CONCRETE PANEL W/ SELECTED COLOR. ARCHITECT AND OWNER SHALL APPROVE PRIOR TO PAINTING REMAINDER OF BUILDING.

ELEVATION COLOR SCHEDULE

<input type="checkbox"/> ① CONCRETE TILT-UP PANEL PAINT	_____
<input type="checkbox"/> ② CONCRETE TILT-UP PANEL PAINT	_____
<input type="checkbox"/> ③ CONCRETE TILT-UP PANEL PAINT	_____
<input type="checkbox"/> ④ CONCRETE TILT-UP PANEL PAINT	_____
<input type="checkbox"/> ⑤ MULLION	COLOR CLEAR ANODIZED
<input type="checkbox"/> ⑥ GLAZING	COLOR BLUE REFLECTIVE
<input type="checkbox"/> ⑦ GLAZING	COLOR METAL CANDY

A. T. AND S.F. RAILROAD



RUSTIN AVENUE

OVERALL SITE PLAN
SCALE: 1" = 10'-0"

SITE PLAN KEYNOTES

- | | |
|---|---|
| 1 HEAVY BRICK FINISH CONC. PARAPET. | 13 HANDICAPPED BIKEWAY SIGN |
| 2 ASPHALT CONCRETE (AC) PAVING | 14 HANDICAPPED PARKING STALL SIGN |
| 3 CONCRETE WALKWAY | 15 NOT USED |
| 4 DRIVEWAY APPROX. TO BE CONSTRUCTED PER "C" & "C" DRAWINGS. | 16 NOT USED |
| 5 4"-8"-4" MIN. THICK CONCRETE EXTERIOR LANDING AND TOP OF ALL EXTERIOR WALK DOORS TO LANDSCAPED AREA. FINISH TO BE MEDIUM BRUSH FINISH. SLOPE TO BE 1/4" : 1" MAX. PROVIDE WALK TO PUBLIC WAY ON ONE WAY 1/20 MAX. AS REQ. BY CITY SPECIFIC. | 17 BIKE RACK |
| 6 NOT USED | 18 LANDSCAPE AREAS |
| 7 NOT USED | 19 CONC. FILLED BOARD POST 6" DIA. (MAX. 48") |
| 8 TRANCHING DONE. | 20 PRE-CAST CONCRETE WHEEL STOP |
| 9 CONCRETE WALK TO THE BUILDING. | 21 6" MIN. FENCE WITH 6" DIA. POST |
| 10 NOT USED | 22 REINFORCE WALL, SEE CHAL |
| 11 EXTERIOR STEEL TRIM. | 23 WASH ENCLOSURE LOCATION OF REQUIRED |
| 12 NOT USED | |

SITE PLAN GENERAL NOTES

- THE SOILS REPORT PREPARED BY _____
- IF SOILS ARE DEEMED IN FAULTIVE, USE STEEL REINFORCING FOR ALL SITE CONCRETE.
- ALL DIMENSIONS ARE TO THE FACE OF CONCRETE WALL. FACE OF CONCRETE CURB OR CURB SIDE CURB.
- SEE "C" DRAWING FOR ALL CONCRETE CURBS, OUTSIDE AND INSIDE. CURBS ON STREET SIDE ARE UNIFORM DIMENSIONS.
- THE EXISTING PROJECT SHALL BE PERMANENTLY MAINTAINED WITH AN AUTOMATIC IRRIGATION SYSTEM. PRIOR TO INSTALLATION A 4" DIA. 90' DIA'S BEFORE SUCC. CONSTRUCTION AS REQUIRED.
- SEE "C" DRAWING FOR POINT OF CONNECTION TO OFF-SITE UTILITY. CONTRACTOR SHALL VERIFY LOCAL UTILITY CONFIGURATION SHALL BE SHOWN BEFORE GROUND BREAKING.
- PROVIDE FURNISH EXCHANGE AREA FROM BLDG. SEE "C" DRAWINGS.
- CONTRACTOR TO REFER TO "C" DRAWING FOR ALL HORIZONTAL CONTROL DIMENSIONS. SITE PLANS ARE FOR CONCRETE AND EXISTING EXISTING POINTS.
- SEE "C" DRAWING FOR FRESH GRADE ELEVATIONS.
- CONCRETE SPECIFICATIONS TO BE A MINIMUM OF 4" THICK W/ TOLDED REINFORCING. IF ALL DIMENSIONS/CONCRETE JOINTS SHALL BE A MINIMUM 1" DIA. 30" W/ 1/2" MAX. SLOPE DIMENSION JOINTS TO MAKE COMPRESSION DOWNWARD FULFILL MATERIAL OF 1/2" SEE "C" DRAWING FOR FINISH.
- PAINT CURBS AND PROVIDE SIGN TO BEHIND OF FIRE LINES AS REQUIRED BY FIRE DEPARTMENT.
- LANDSCAPED AREAS SHALL BE DELINEATED WITH A MINIMUM 2X4 INCHES (2") WOOD CURB.
- APPROVED CONCEPTUAL LANDSCAPE PLAN PRIOR TO GRADING PERMIT.

SITE LEGEND

- | | |
|---|--|
| [Pattern] LANDSCAPED AREA | [Pattern] STANDARD PARKING STALL (8' X 14') |
| [Pattern] AC PAVING - SEE "C" DRAWING FOR THICKNESS | [Pattern] HANDICAPPED PARKING STALL (8' X 14') |
| [Pattern] CONCRETE FINISH - SEE "C" DRAWING FOR THICKNESS | [Pattern] PUSH OF TRIM |
| [Pattern] EXISTING PAVED TRAIL | |

HPANEVLOGO-09.jpg

1000 P.C.
11211 business center - ste.
P.O. Box 1000
80212
Tel: 303-452-1770
Fax: 303-452-0801
Email: hpa@hpa.com

Owner

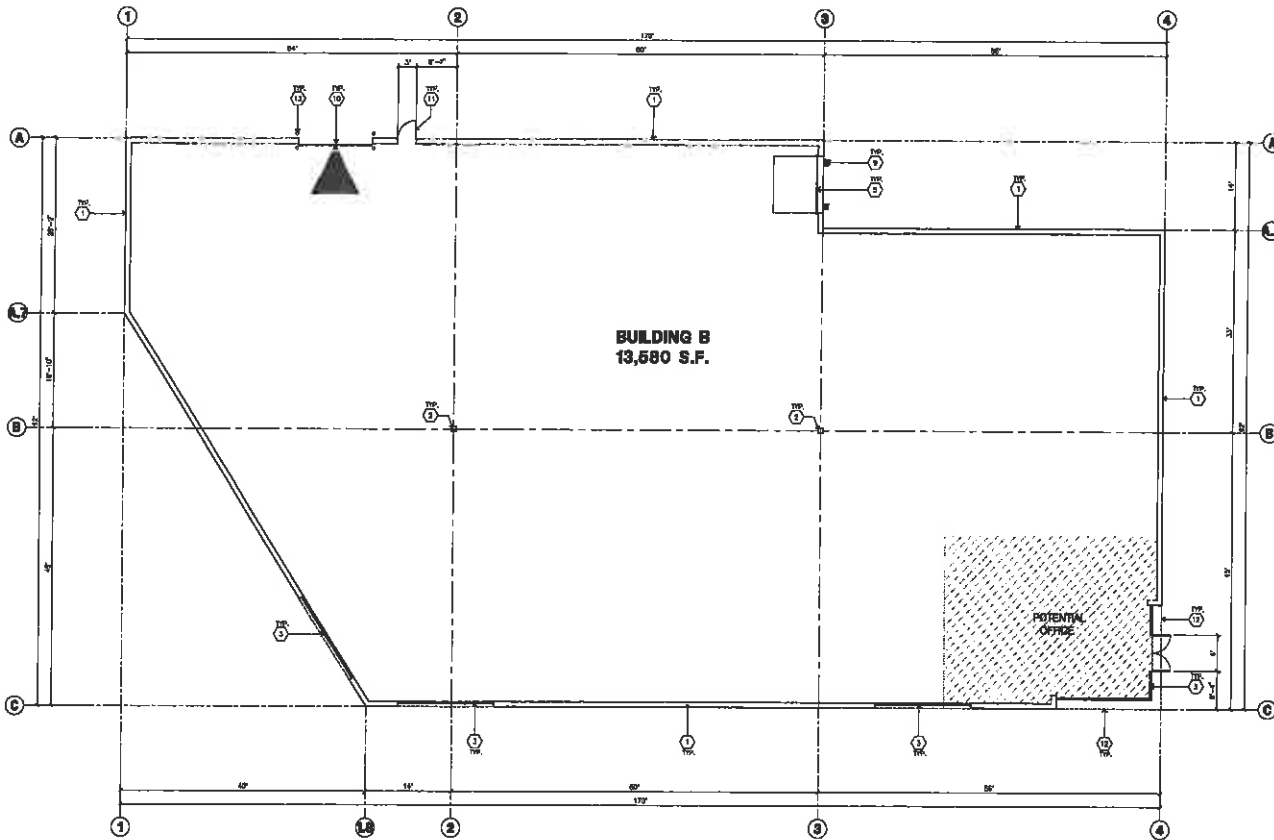
Project

Consultants

TRM: overall plan

Project Number:
Drawn by:
Date:
Revision:

Sheet:
B-A1.1



KETNOTES - FLOOR PLAN

- 1) CONCRETE TIE-UP PANEL.
- 2) STRUCTURE, STEEL COLUMN.
- 3) TYPICAL REINFORCEMENT SPACING WITH SLABING. SEE OFFICE SLOPE-UP AND REINFORCEMENT FOR WALL, COLUMN AND LOGGERS.
- 4) REINFORCEMENT OF WALL, SECTION TYPICAL. SEE WALL ON SLABING WALL ON SECTION.
- 5) 4'-0" x 4'-0" BRICK DOOR, SLOPED GLASS, STAINLESS STEEL.
- 6) CONCRETE CONCRETE WALL.
- 7) 2'-0" x 2'-0" TYPICAL CONCRETE EXTERIOR LANDING AND FINISH. AT ALL CORNERS SEE PLAN FOR LANDING WALL, FINISH BY INTERIOR EXTERIOR FINISH, SLAB TO FINISH 1/2" FROM FINISH. FINISH BY INTERIOR EXTERIOR FINISH.
- 8) CONCRETE CONCRETE WALL.
- 9) 12" x 14" DRIVE PANEL SECTIONAL OIL, BROWARD COUNTY.
- 10) 30" x 30" HOLLOW METAL EXTERIOR MAIN DOOR.
- 11) 30" x 30" HOLLOW METAL EXTERIOR MAIN DOOR.
- 12) 30" x 30" HOLLOW METAL EXTERIOR MAIN DOOR.
- 13) 30" x 30" HOLLOW METAL EXTERIOR MAIN DOOR.
- 14) 30" x 30" HOLLOW METAL EXTERIOR MAIN DOOR.
- 15) 30" x 30" HOLLOW METAL EXTERIOR MAIN DOOR.
- 16) 30" x 30" HOLLOW METAL EXTERIOR MAIN DOOR.
- 17) 30" x 30" HOLLOW METAL EXTERIOR MAIN DOOR.
- 18) 30" x 30" HOLLOW METAL EXTERIOR MAIN DOOR.
- 19) 30" x 30" HOLLOW METAL EXTERIOR MAIN DOOR.
- 20) 30" x 30" HOLLOW METAL EXTERIOR MAIN DOOR.
- 21) 30" x 30" HOLLOW METAL EXTERIOR MAIN DOOR.
- 22) 30" x 30" HOLLOW METAL EXTERIOR MAIN DOOR.
- 23) 30" x 30" HOLLOW METAL EXTERIOR MAIN DOOR.
- 24) 30" x 30" HOLLOW METAL EXTERIOR MAIN DOOR.
- 25) 30" x 30" HOLLOW METAL EXTERIOR MAIN DOOR.
- 26) 30" x 30" HOLLOW METAL EXTERIOR MAIN DOOR.
- 27) 30" x 30" HOLLOW METAL EXTERIOR MAIN DOOR.
- 28) 30" x 30" HOLLOW METAL EXTERIOR MAIN DOOR.
- 29) 30" x 30" HOLLOW METAL EXTERIOR MAIN DOOR.
- 30) 30" x 30" HOLLOW METAL EXTERIOR MAIN DOOR.

GENERAL NOTES - FLOOR PLAN

- A. THIS BUILDING IS DESIGNED FOR LEAD PILE STIFFNESS WITH THE DESIGN AND DESIGN OF THE FOUNDATION IS TO BE PROVIDED BY THE ARCHITECT'S SPECIALIST.
- B. THE FLOOR LOADINGS SHALL BE APPROVED BY THE COMMISSIONER.
- C. THE BUILDING FLOOR SHALL BE FINISHED WITH "C" FINISHES FOR FRESH CONCRETE FINISHES.
- D. NOT USED.
- E. ALL INTERIOR WALLS AND PARTITIONS SHALL BE FINISHED WITH GYP-SHE DRYWALL AND FINISHED WITH PLASTER/STUCCO, COORDINATE WITH ARCHITECT'S SPECIALIST.
- F. ALL INTERIOR WALLS AND PARTITIONS SHALL BE FINISHED WITH GYP-SHE DRYWALL AND FINISHED WITH PLASTER/STUCCO, COORDINATE WITH ARCHITECT'S SPECIALIST.
- G. ALL INTERIOR WALLS AND PARTITIONS SHALL BE FINISHED WITH GYP-SHE DRYWALL AND FINISHED WITH PLASTER/STUCCO, COORDINATE WITH ARCHITECT'S SPECIALIST.
- H. ALL INTERIOR WALLS AND PARTITIONS SHALL BE FINISHED WITH GYP-SHE DRYWALL AND FINISHED WITH PLASTER/STUCCO, COORDINATE WITH ARCHITECT'S SPECIALIST.
- I. ALL INTERIOR WALLS AND PARTITIONS SHALL BE FINISHED WITH GYP-SHE DRYWALL AND FINISHED WITH PLASTER/STUCCO, COORDINATE WITH ARCHITECT'S SPECIALIST.
- J. ALL INTERIOR WALLS AND PARTITIONS SHALL BE FINISHED WITH GYP-SHE DRYWALL AND FINISHED WITH PLASTER/STUCCO, COORDINATE WITH ARCHITECT'S SPECIALIST.
- K. ALL INTERIOR WALLS AND PARTITIONS SHALL BE FINISHED WITH GYP-SHE DRYWALL AND FINISHED WITH PLASTER/STUCCO, COORDINATE WITH ARCHITECT'S SPECIALIST.
- L. ALL INTERIOR WALLS AND PARTITIONS SHALL BE FINISHED WITH GYP-SHE DRYWALL AND FINISHED WITH PLASTER/STUCCO, COORDINATE WITH ARCHITECT'S SPECIALIST.
- M. ALL INTERIOR WALLS AND PARTITIONS SHALL BE FINISHED WITH GYP-SHE DRYWALL AND FINISHED WITH PLASTER/STUCCO, COORDINATE WITH ARCHITECT'S SPECIALIST.
- N. ALL INTERIOR WALLS AND PARTITIONS SHALL BE FINISHED WITH GYP-SHE DRYWALL AND FINISHED WITH PLASTER/STUCCO, COORDINATE WITH ARCHITECT'S SPECIALIST.
- O. ALL INTERIOR WALLS AND PARTITIONS SHALL BE FINISHED WITH GYP-SHE DRYWALL AND FINISHED WITH PLASTER/STUCCO, COORDINATE WITH ARCHITECT'S SPECIALIST.
- P. ALL INTERIOR WALLS AND PARTITIONS SHALL BE FINISHED WITH GYP-SHE DRYWALL AND FINISHED WITH PLASTER/STUCCO, COORDINATE WITH ARCHITECT'S SPECIALIST.
- Q. ALL INTERIOR WALLS AND PARTITIONS SHALL BE FINISHED WITH GYP-SHE DRYWALL AND FINISHED WITH PLASTER/STUCCO, COORDINATE WITH ARCHITECT'S SPECIALIST.
- R. ALL INTERIOR WALLS AND PARTITIONS SHALL BE FINISHED WITH GYP-SHE DRYWALL AND FINISHED WITH PLASTER/STUCCO, COORDINATE WITH ARCHITECT'S SPECIALIST.
- S. ALL INTERIOR WALLS AND PARTITIONS SHALL BE FINISHED WITH GYP-SHE DRYWALL AND FINISHED WITH PLASTER/STUCCO, COORDINATE WITH ARCHITECT'S SPECIALIST.
- T. ALL INTERIOR WALLS AND PARTITIONS SHALL BE FINISHED WITH GYP-SHE DRYWALL AND FINISHED WITH PLASTER/STUCCO, COORDINATE WITH ARCHITECT'S SPECIALIST.
- U. ALL INTERIOR WALLS AND PARTITIONS SHALL BE FINISHED WITH GYP-SHE DRYWALL AND FINISHED WITH PLASTER/STUCCO, COORDINATE WITH ARCHITECT'S SPECIALIST.
- V. ALL INTERIOR WALLS AND PARTITIONS SHALL BE FINISHED WITH GYP-SHE DRYWALL AND FINISHED WITH PLASTER/STUCCO, COORDINATE WITH ARCHITECT'S SPECIALIST.
- W. ALL INTERIOR WALLS AND PARTITIONS SHALL BE FINISHED WITH GYP-SHE DRYWALL AND FINISHED WITH PLASTER/STUCCO, COORDINATE WITH ARCHITECT'S SPECIALIST.
- X. ALL INTERIOR WALLS AND PARTITIONS SHALL BE FINISHED WITH GYP-SHE DRYWALL AND FINISHED WITH PLASTER/STUCCO, COORDINATE WITH ARCHITECT'S SPECIALIST.
- Y. ALL INTERIOR WALLS AND PARTITIONS SHALL BE FINISHED WITH GYP-SHE DRYWALL AND FINISHED WITH PLASTER/STUCCO, COORDINATE WITH ARCHITECT'S SPECIALIST.
- Z. ALL INTERIOR WALLS AND PARTITIONS SHALL BE FINISHED WITH GYP-SHE DRYWALL AND FINISHED WITH PLASTER/STUCCO, COORDINATE WITH ARCHITECT'S SPECIALIST.

OVERALL FLOOR PLAN
SCALE: 1/8"=1'-0"
DATE: 1/20/11

N:\PANEV\1000-09.dwg
Tpm, Inc.
16211 BAYVIEW AVENUE - 5th.
6150 PLYM, CA
90512
Tel: 949-452-1772
Fax: 949-452-0651
email: tpm@tpmaha.com

Owner:

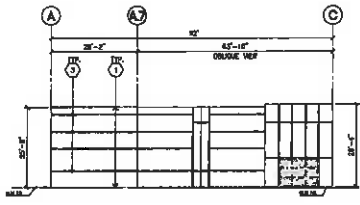
Project:

Consultants:

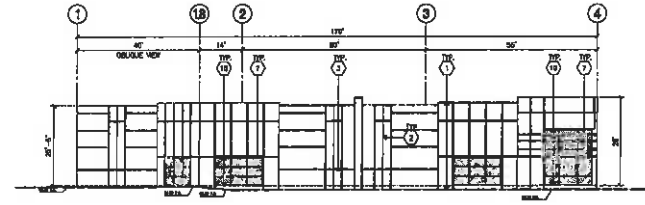
Title: Overall Floor Plan

Project Number:
Drawn by:
Date:
Revised:

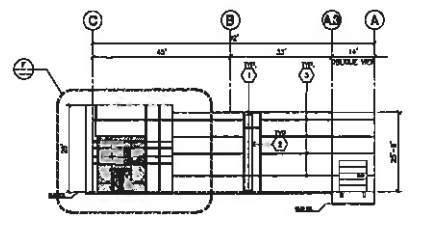
Sheet:
B-A2.1



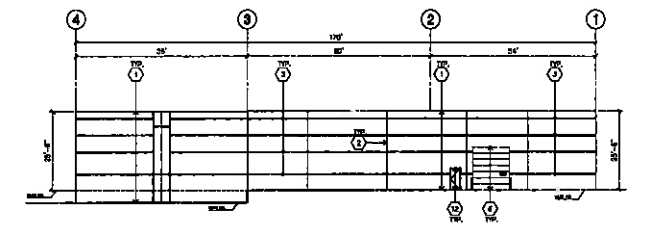
NORTH ELEVATION
SCALE: 1/4" = 1'-0"



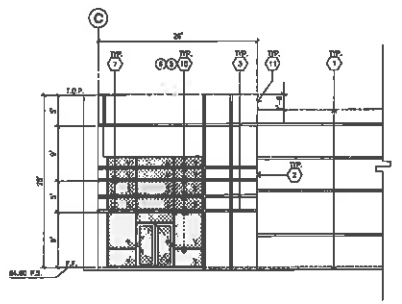
WEST ELEVATION
SCALE: 1/4" = 1'-0"



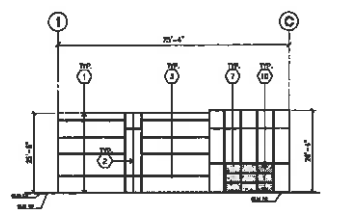
SOUTH ELEVATION
SCALE: 1/4" = 1'-0"



EAST ELEVATION
SCALE: 1/4" = 1'-0"



ENLARGED SOUTH ELEVATION
SCALE: 1/4" = 1'-0"



NORTHWEST ELEVATION
SCALE: 1/4" = 1'-0"

ELEVATION KEYNOTES

- ① CONCRETE TILT-UP PANEL (PAINTED)
- ② PANEL JOINT
- ③ PANEL FINISH
- ④ METAL LUGGER
- ⑤ 2'-0" X 10" OVERHEAD DOOR @ DOCK HIGH
- ⑥ 12" X 16" OVERHEAD DOOR @ DRIVE THRU
- ⑦ METAL TUBE STEEL ALUMINUM FINISHED METAL PANEL STUDS
- ⑧ CONCRETE STAIR, LANDING AND CONC. GUARDRAIL
- ⑨ DOCK BUMPER
- ⑩ ALUMINUM STOREFRONT FRAMING W/ TIGHTFIT GLAZING AT ALL DOORS. SPIGETS ADJACENT TO DOORS AND GLAZING W/ BOTTOMS LESS THAN 18" ABOVE F.F. ELEVATION. PAINTED INTERIOR
- ⑪ HOLLOW METAL DOOR
- ⑫ EXTERIOR DOWNSPOUT AND 2 OVERFLOW SCUPPERS
- ⑬ INTERIOR DOWNSPOUT AND 2 OVERFLOW SCUPPERS

GLAZING LEGEND

- WOOD GLAZING
- ALUMINUM GLAZING

ALL GLAZING TO BE TIGHTFIT
GLAZING PER WINDOW, DO NOTICE A BELIEVER IN DO CLEAR
1" GAP, 3/8" G.S., 1/2" SHG
1" BELIEVER GLASS SHG W/ 1/2" AIRSPACE AND 0/1 1/4" LITE
MULLIONS: INTERIOR CLEAR

ELEVATION GENERAL NOTES

- ALL PAINT COLOR CHANGES TO OCCUR AT INSIDE CORNERS UNLESS NOTED OTHERWISE.
- ALL PAINT FINISHES ARE TO BE FLAT UNLESS NOTED OTHERWISE.
- T.O.P. = TOP OF FINISH - ELEVATION.
- F.F. = FINISH FLOOR ELEVATION.
- STOREFRONT CONSTRUCTION: GLASS, METAL ATTACHMENTS AND UNITS TO BE DESIGNED TO RESIST 90 MPH EXPOSURE "U" WIND. CONTRACTOR TO SUBMIT SHOP DRAWINGS PRIOR TO INSTALLATION.
- CONTRACTOR SHALL FULLY PAINT ONE CONCRETE PANEL BY SELECTED COLOR. ARCHITECT AND OWNER SHALL APPROVE PRIOR TO PAINTING REMAINDER OF BUILDING.

ELEVATION COLOR SCHEDULE

<input type="checkbox"/> ① CONCRETE TILT-UP PANEL, PAINT	_____
<input type="checkbox"/> ② CONCRETE TILT-UP PANEL, PAINT	_____
<input type="checkbox"/> ③ CONCRETE TILT-UP PANEL, PAINT	_____
<input type="checkbox"/> ④ CONCRETE TILT-UP PANEL, PAINT	_____
<input type="checkbox"/> ⑤ MULLIONS	COLOR CLEAR ANODIZED
<input type="checkbox"/> ⑥ GLAZING	COLOR BLUE REFLECTIVE
<input type="checkbox"/> ⑦ GLAZING	COLOR METAL LUGGER



Hpa, Inc.
16231 BERING AVENUE - 2ND
P.O. BOX 104
92012
Tel: 949-484-1770
Fax: 949-484-0851
email: hpa@hpa.com

Owner:

Project:

Consultants:

- Civil
- Structural
- Mechanical
- Plumbing
- Electrical
- Landscaping
- Fire Protection
- Soils/Geotech

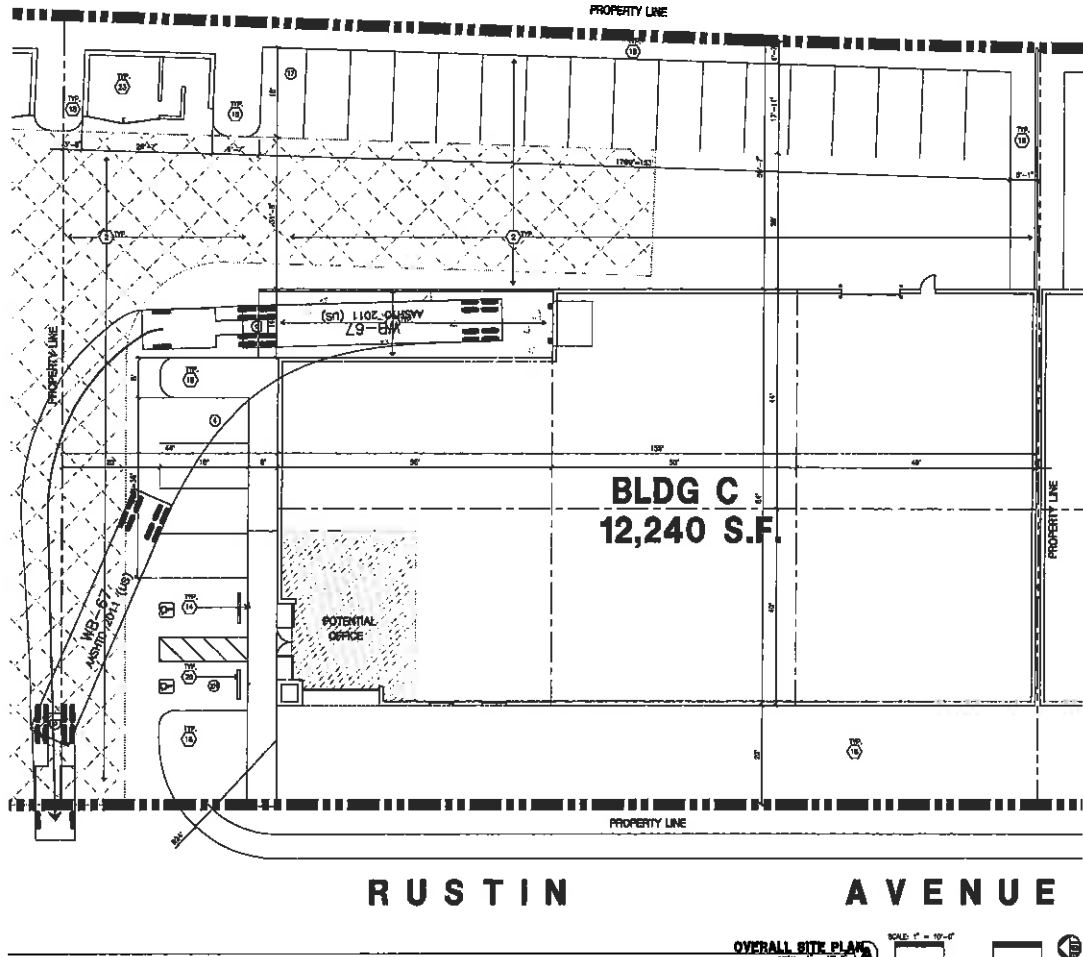
Title: _____
Elevation: _____

Project Number: _____
Drawn by: _____
Date: _____
Description: _____

Sheet:

A3.1

A. T. AND S. F. RAILROAD



RUSTIN AVENUE

OVERALL SITE PLAN
SCALE: 1/4" = 10'-0"
DATE: 10/20/11

SITE PLAN KEYNOTES

1. HEAVY BRUSH FINISH CONCRETE PAVEMENT.
2. ASPHALT CONCRETE (AC) PAVING.
3. CONCRETE SIDEWALK.
4. SIDEWALK APPROX. TO BE CONSTRUCTED PER "C" & "C" DRAWINGS.
5. 8"-8 1/2" x 8 1/2" x 4" MIN. THICK CONCRETE EXTERIOR LANDSCAPE AND TYP. OF ALL EXTERIOR WALK SURFACES TO LANDSCAPED AREAS. FINISH TO BE MEDIUM BRUSH FINISH. SLOPE TO BE 1/4" = 1' MIN. PROVIDE WALK TO PUBLIC WAY OR DRIVE WAY 1/2" TO 3/4" MAX. AS REQ. BY CITY REQUIRE.
6. NOT USED.
7. NOT USED.
8. TELEGRAPHIC DOME.
9. CONCRETE RAMP TO THE BUILDING.
10. NOT USED.
11. EXTERIOR STEEL WALK.
12. NOT USED.

13. UNDEVELOPED ENTRY SIGN.
14. UNDEVELOPED PARKING SIGN, SIGN.
15. NOT USED.
16. NOT USED.
17. BONE FINISH.
18. LANDSCAPE PLANT.
19. CONCRETE FILLER CONCRETE POST 6" DIA. MAX. 42" H.
20. PRE-CAST CONCRETE WHEEL STOP.
21. 8' H. WOOD FENCE WITH 8' H. GATE.
22. RETAINING WALL. SEE CIVIL.
23. TRASH ENCLOSURE (LOCATION OF FUTURE).

SITE PLAN GENERAL NOTES

1. THE SOIL REPORT PREPARED BY _____.
2. IF WELLS ARE DEVELOPED BY RETAIN. USE STEEL REINFORCING FOR ALL SITE CONCRETE.
3. ALL CONCRETE ARE TO THE FACE OF CONCRETE WALL. FACE OF CONCRETE CORNER ON ONE LINE BASIS.
4. SEE "C" PLANS FOR ALL CONCRETE WALL, SLABS AND CHALK. DETAILS ON SHEET SET ARE USUALLY WITHDRAWN.
5. THE CIVIL ENGINEER SHALL BE RESPONSIBLE FOR PROVIDING WITH AN APPROVED CONCEPTUAL LANDSCAPE PLAN WITH TO CONFORM WITH 10 DAYS BEFORE BUILDING COMPLETION AS REQUIRED.
6. SEE "C" DRAWINGS FOR POINT OF DIMENSIONS TO INTERIOR. INTERIOR DIMENSIONS SHALL VERIFY ACTUAL UTILITY CONNECTION SHALL VERIFY ACTUAL UTILITY LOCATIONS.
7. PHONE POLES ORANGE AWAY FROM BLDG. SEE "C" DRAWINGS.
8. CONTRACTOR TO REFER TO "C" DRAWINGS FOR ALL HORIZONTAL CONTROL DIMENSIONS. SEE PLANS FOR CLEARANCE AND SETBACK LAYOUT POINTS.
9. SEE "C" DRAWINGS FOR FINISH GRADE ELEVATIONS.
10. CONCRETE SHALL BE TO A MINIMUM OF 4" THICK W/ 100LB CORNER IF 8" DIA. CONCRETE/CONCRETE SHALL BE A MINIMUM 12" DIA. W/ 120 LB. CONC. DIMENSIONS SHALL TO ONE CONCRETE DIMENSION FILLER MINIMUM OF 1/4". SEE "C" DRAWINGS FOR FINISH.
11. PAINT COLORED AND PROMISE DONE TO INTERNAL OF FIRE LINES AS REQUIRED BY FIRE DEPARTMENT.
12. LANDSCAPED AREA SHALL BE DELINEATED WITH A MINIMUM 3/8" WIDE 6" H. FINISH CURB.
13. APPROVED CONCEPTUAL LANDSCAPE PLAN PRIOR TO GRADING POINT.

SITE LEGEND

- LANDSCAPED AREA
- AC PAVING - SEE "C" DRAWING FOR THICKNESS
- CONCRETE PAVING - SEE "C" DRAWING FOR THICKNESS
- EXTERIOR PAVED DRIV.
- STANDARD PARKING STALL (P X 19')
- LANDSCAPED PARKING STALL (P X 19')
- PATH OF WALK

HPANEVLOGO-09_09

Inc. Inc.
11311 Michigan Avenue - 1A
#100 Irvine, CA
92612
Tel: 949-452-1773
Fax: 949-253-0861
Email: hpa@hpa.com

Owner

Project

Consultants

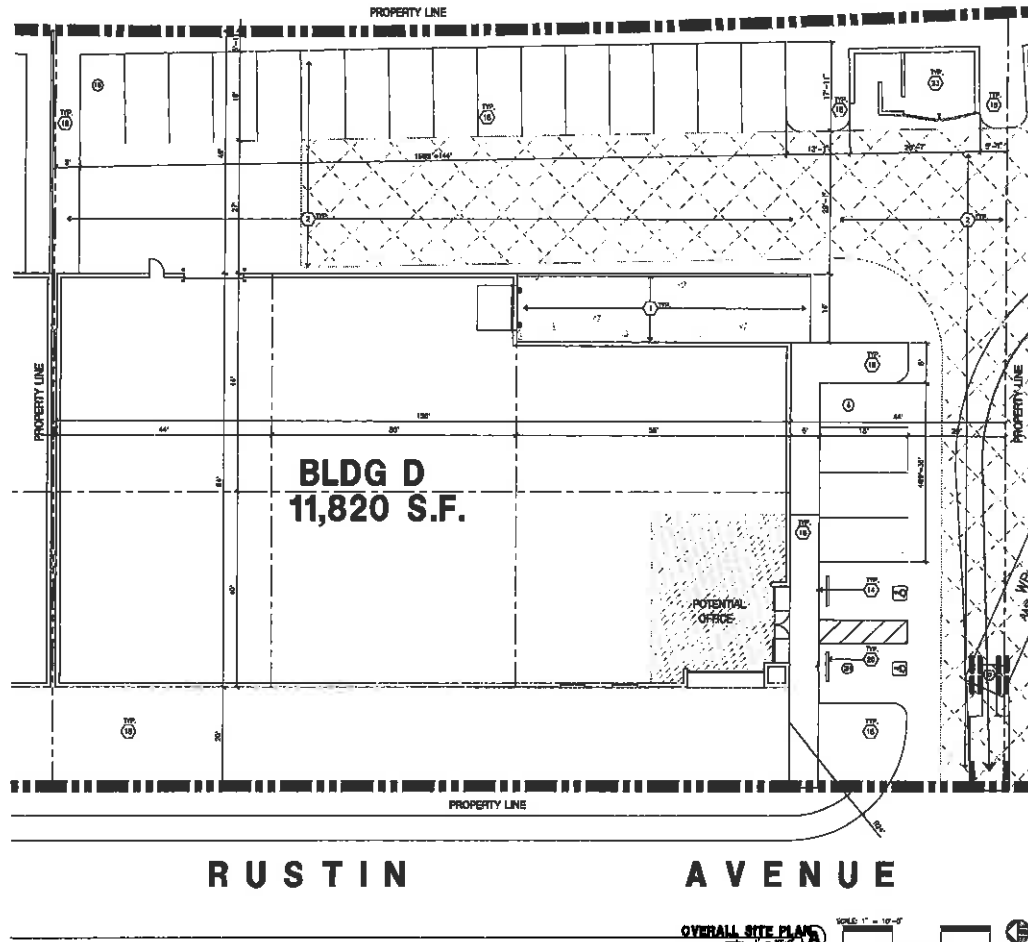
Title: overall site plan

Project Number
Drawn by:
Date:
Revisions:

Sheet:

C-A1.1

A. T. AND S. F. RAILROAD



SITE PLAN KEYNOTES

- | | |
|---|---|
| <ul style="list-style-type: none"> 1 HEAVY BRICK FRESH CONC. PARAPET. 2 ASPHALT CONCRETE (AC) PAVING 3 CONCRETE BALANBY 4 CONCRETE APPROX. TO BE CONSTRUCTED FOR "C" DRAWINGS. 5 3'-0" x 3'-0" x 18" THICK CONCRETE EXTERIOR LANDING AND TYP. AT ALL EXTERIOR WALK SURFACES TO LANDSCAPED AREAS. FINISH TO BE MEDIUM BRICK PAVING. SLOPE TO BE 1/4" x 12" MAX. PROVIDE WALK TO PUBLIC SWAY OR DRIVE WAY BY 1/2" MAX. AS REQ. BY CITY INSPECTOR. 6 NOT USED 7 NOT USED 8 UNPAVED DRIVE 9 CONCRETE RAMP TO THE BUILDING. 10 NOT USED 11 EXTERIOR STEEL STAIR. 12 NOT USED | <ul style="list-style-type: none"> 13 UNDEVELOPED EXIST. SOIL 14 UNDEVELOPED PAVING SHALL SOIL 15 NOT USED 16 NOT USED 17 BRICK ROCK 18 LANDSCAPE AREAS 19 CONC. FILLED CURVED PORT #1 OR, UNLTD. 42" H 20 PRE-CAST CONCRETE TRUSS STOP 21 6" H. WALL FENCE WITH 6" H. GATE 22 REMAINING WALL, SEE CHL. 23 WIND ENCLASURE LOCATION (IF REQUIRED) |
|---|---|

SITE PLAN GENERAL NOTES

1. THE SOIL REPORT PROVIDED BY _____
2. IF SOILS ARE UNSATISFACTORY IN NATURE, USE STEEL REINFORCING FOR ALL SITE CONCRETE.
3. ALL DIMENSIONS ARE TO THE FACE OF CONCRETE WALL. FACE OF CONCRETE CURB OR WIND LINE UNLTD.
4. SEE "C" PLANS FOR ALL CONCRETE CURBS, GUTTERS AND SHOULDS. DETAILS ON SHEET A-01 ARE MANDATORY.
5. THE EXISTING PROPOSED SHALL BE IMMEDIATELY SURVEILLED WITH AN AUTOMATIC LEVELING SYSTEM. PROVIDE TO SUBMITTALS A SET OF LATEST AS BUILT SURVEY DATA. CONSTRUCTION SHALL VERIFY ACTUAL UTILITY LOCATIONS SHALL VERIFY ACTUAL UTILITY LOCATIONS.
6. PROVIDE POSITIVE DRAINAGE AWAY FROM BLDG. SEE "C" DRAWINGS.
7. CONTRACTOR TO REFER TO "C" DRAWINGS FOR ALL NON-CONCRETE CURBS, GUTTERS, SITE PLANS AND FOR CURBING AND EXISTING LANDSCAPING.
8. SEE "C" DRAWINGS FOR FRESH DRINK BLDG. DETAILS.
9. CONCRETE REINFORCE TO BE A MINIMUM OF 4" THICK W/ TYPED CURBS AT 18" O.C. EXISTING/CONSTRUCTION JOINTS SHALL BE A MINIMUM 12" O.C. MAX W/ 12" MAX. SLOPE. EXISTING JOINTS TO HAVE COMPRESSIVE STRENGTH FULLY MINIMUM OF 1/4" SEC "C" DRAWINGS FOR FRESH.
10. PAINT CURBS AND PROVIDE SIGN TO INDICATE THE LINES AS REQUIRED BY FIRE DEPARTMENT.
11. LANDSCAPED AREAS SHALL BE DELINEATED WITH A MINIMUM 18" HEIGHT WOOD CURB.
12. APPROVED CONCEPTUAL LANDSCAPE PLAN PRIOR TO GRADING PERMIT.

SITE LEGEND

- | | |
|--|--|
| <ul style="list-style-type: none"> LANDSCAPED AREA AC PAVING - SEE "C" DRAWINGS, FIRE DEPARTMENT CONCRETE PAVING - SEE "C" DRAWINGS FOR FINISHES EXISTING PAVED ROAD | <ul style="list-style-type: none"> UNPAVED PAVING - SEE "C" DRAWINGS HANDICAP PARKING - SEE "C" DRAWINGS PATH OF TRAVEL |
|--|--|

HPANE\W.DGO-09.dwg

HPA, Inc.
12321 Madison Avenue - 12A,
#100 Irvine, CA
92612
Tel: 949-453-1770
Fax: 949-453-0851
Email: hpa@hpa.com

Owner:

Project:

Consultants:

Title: overall site plan

Project Number: _____
Drawn by: _____
Date: _____
Revised: _____

Sheet:

D-A1.1

Owner:

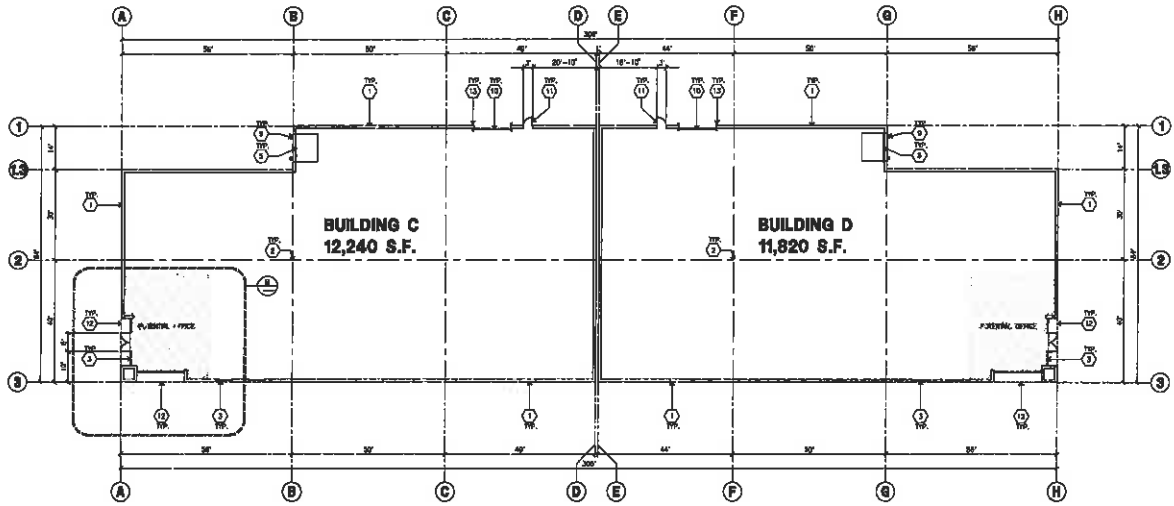
Project:

Consultants:

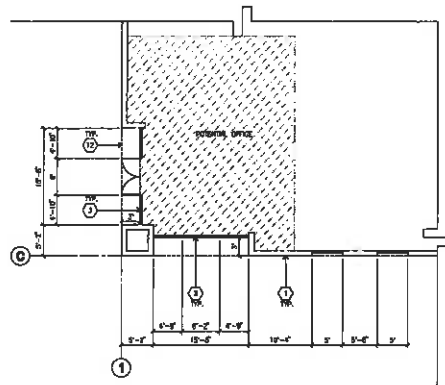
Title: Overall Floor Plan

Project Number: -
Drawn by: -
Date: -
Revision: -

Sheet:
CD-A2.1



OVERALL FLOOR PLAN
SCALE: 1/8"=1'-0"



ENLARGED FLOOR PLAN
SCALE: 1/8"=1'-0"

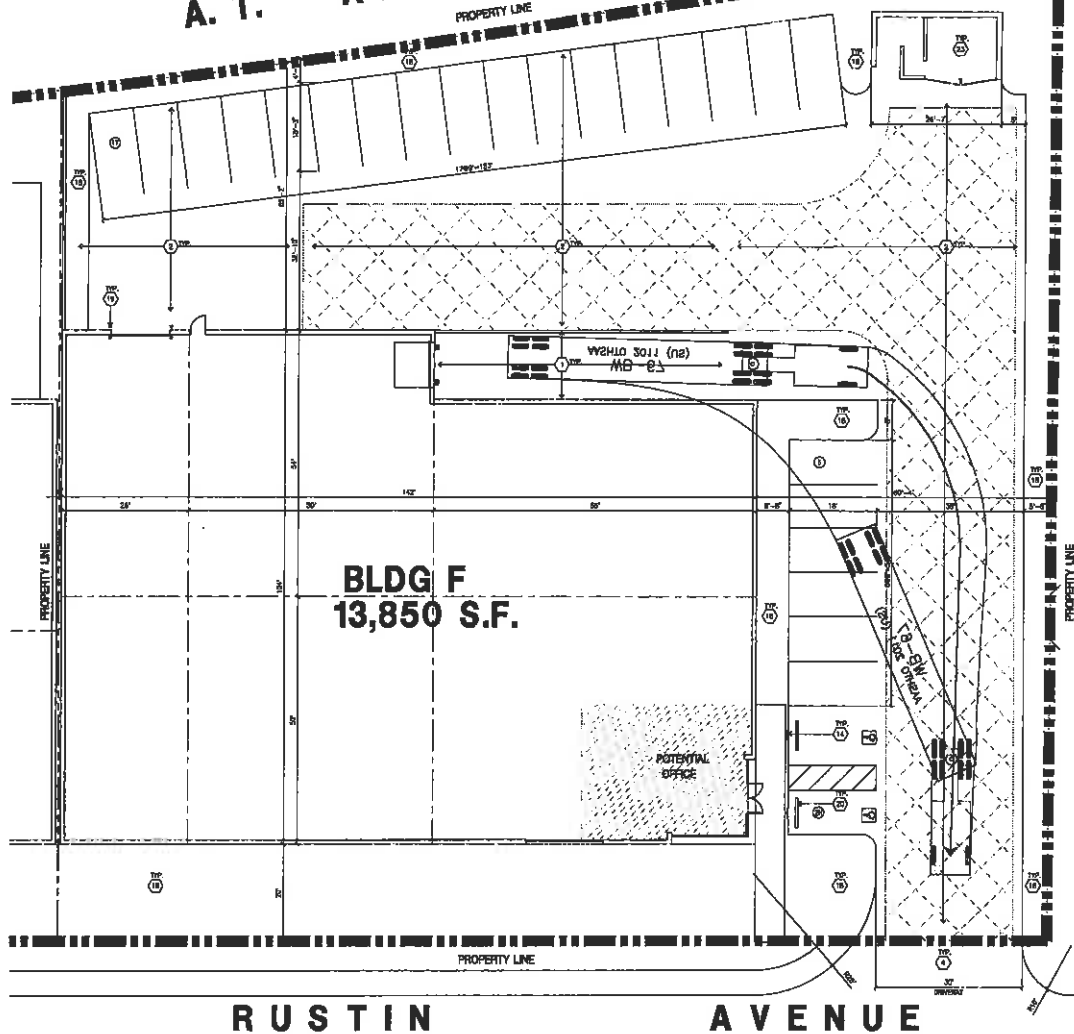
KEYNOTES - FLOOR PLAN

- 1 CONCRETE SLAB-FLOOR PANEL.
- 2 STRUCTURAL STEEL COLUMN.
- 3 CONCRETE FLOORING FINISH WITH SLAB. SEE OFFICE SLAB-UP AND ELEVATIONS FOR SPEC. COLOR AND LOCATION.
- 4 CONCRETE FLOOR FIN. EXTEND CONC. TILT-UP CURB WALL OR BALCONY WALL UP WITH FIN. OF SLAB.
- 5 4'-1/2" x 12" TRUCK DOOR, SECTION, C/W. SHOWN OPEN.
- 6 EXTERIOR CONCRETE FINISH.
- 7 FINISH FLOOR FINISH CONCRETE FINISHING NOT TYPICAL AT ALL EXTERIOR WALL JOINTS TO UNDERLAYS AND FINISH UP BY FINISH FLOOR FINISH. SEE 101 FOR FIN. FLOOR FINISHING. THIS TO BE SHOWN FOR OFF. BUILDINGS.
- 8 LAMINATE FINISH FOR VENTILATION.
- 9 DOOR EIGHT DOOR.
- 10 12" x 12" CONC. PANEL SECTION, C/W. SHOWN OPEN.
- 11 2"X4" HOLDUP WALL, EXTERIOR WALL DOOR.
- 12 2"X4" HOLDUP WALL.
- 13 CONC. FLOOR SLAB FIN. AT ALL WALLS. 4" DIA.
- 14 EXTERIOR ROOF/WALL WITH OVERLAP ROOFING.
- 15 2" CURB.

GENERAL NOTES - FLOOR PLAN

- A. THIS BUILDING IS DESIGNED FOR VEHICULAR STORAGE WITH FIRE PROTECTION AND CODES OF THE JURISDICTION. A SEPARATE PERMIT WILL BE REQUIRED FOR ANY MODIFICATION THEREOF.
- B. FIRE HOSE CONNECTION SHALL BE PROVIDED FOR EACH BUILDING.
- C. THE BUILDING SLAB IS 2.5" THICK. SEE "C" DIMENSIONS FOR FINISH FLOOR ELEVATIONS.
- D. NOT USED.
- E. THIS BUILDING IS DESIGNED FOR VEHICULAR STORAGE WITH FIRE PROTECTION AND CODES OF THE JURISDICTION. A SEPARATE PERMIT WILL BE REQUIRED FOR ANY MODIFICATION THEREOF.
- F. ALL DIMENSIONS ARE TO THE FACE OF CONCRETE PANEL UNLESS NOTED TO THE CONTRARY.
- G. SEE CIVIL DRAWING FOR POINT OF CONNECTIONS TO OFF-SITE SERVICES. CONSULT WITH APPROPRIATE AGENCIES.
- H. FOR CODE TYPES AND RATES, SEE CODES, SPEC. AND A/C. HAVE ALL DRAWINGS FOR CODES REVIEWED BY LOCAL OFFICIALS.
- I. CONSTRUCTION IS LIMITED AND KEEP THE FLOOR SLAB CLEAR. ALL EQUIPMENT TO BE SHOWN HOLDING OVER AND BLACK.
- J. ALL DOOR AND WINDOW OPENINGS TO HAVE ALLOWED EXT. SEAL, WINDOW, OR GLASS.
- K. FINISH FLOORING AND CONCRETE FINISHING SHALL NOT BE USED OR SPECIFIED IN THIS DRAWING.
- L. THE EXTERIOR FINISH SHALL BE SHOWN BY A SYMBOL FOR THE FINISH TYPE AND COLOR.
- M. THE EXTERIOR FINISH OF THE BUILDING SHALL BE SHOWN BY A SYMBOL FOR THE FINISH TYPE AND COLOR.
- N. NON-FINISHING CODES. FINISHING SHALL BE SHOWN BY A SYMBOL FOR THE FINISH TYPE AND COLOR.
- O. ALL ROOF FINISHING SHALL BE FULLY SHOWN FROM FINISH VIEW. SEE ADJACENT OFFICE DRAWING.

A. T. AND S. F. RAILROAD



RUSTIN AVENUE

OVERALL SITE PLAN
SCALE: 1" = 10'-0"

SITE PLAN KEYNOTES

- 1. NEARLY BROOM FINISH CONC. PAVEMENT.
- 2. ASPHALT CONCRETE (AC) PARKING
- 3. MONUMENT WALKWAY
- 4. DRIVEWAY ASPHALT TO BE CONSTRUCTED FOR "1.5" "C" DRAINAGE.
- 5. 3"-4" (30"-42") MIN. THICK CONCRETE EXTERIOR LANDING AND TOP OF ALL EXTERIOR WALLS EXPOSE TO UNDESIGNED AREA. FINISH TO BE LEVEL BROOM FINISH. SLOPE TO BE 1/4" = 1" MIN. FINISH SHALL BE FIELD SET ON DRIVE WAY BY 1500 MAG. AS REQ. BY CITY INSPECTOR.
- 6. NOT USED
- 7. NOT USED
- 8. TRUNCATED DOME
- 9. CONCRETE READY TO BE BURNED.
- 10. NOT USED
- 11. EXTERIOR STEEL STRIP
- 12. NOT USED
- 13. HANDICAPPED ENTRY SOH
- 14. HANDICAPPED PARKING STEEL SIGN
- 15. NOT USED
- 16. NOT USED
- 17. BASE RACK
- 18. LANDSCAPE AREA
- 19. COAL FILLED GRAVED POST IF DIA. 1/2" DIA. 12" H.
- 20. PRE-CAST CONCRETE WIND STOP
- 21. 8" H. CONC. FENCE WITH 8" DIA. CHAIN LINK
- 22. RETAINING WALL, SIX (6) IN.
- 23. TRASH ENCLOSURE LOCATION (IF REQUIRED)

SITE PLAN GENERAL NOTES

1. THE SOILS REPORT PREPARED BY _____
2. IF SOILS ARE UNSATISFACTORY IN NATURE, USE STEEL REINFORCING FOR ALL FIVE CONCRETE.
3. ALL DIMENSIONS ARE TO THE FACE OF CONCRETE WALL. FACE OF CONCRETE SHALL BE 3/4" LINE TOLERANCE.
4. SEE "C" PLAN FOR ALL EXTERIOR CORNER, EXTERIOR AND INTERIOR. DETAILS ON SHEET A-1.1 ARE PRESENT STANDARDS.
5. THE EXISTING PROJECT SHALL BE PERMANENTLY MAINTAINED WITH ALL AUTOMATIC REGISTRATION SYSTEM FROM TO REGISTRATION IS AT LEAST 90 DAYS BEFORE OLD. COMPLETION AS REQUIRED.
6. SEE "C" DRAWINGS FOR NOTES OF DIMENSIONS TO OFF-GRID. OFF-GRID DIMENSIONS SHALL BE TO ACTUAL UTILITY CONTRACTOR SHALL VERIFY ACTUAL UTILITY LOCATIONS.
7. PROVIDE POSITIVE DRAINAGE AWAY FROM BLDG. SEE "C" DRAWINGS.
8. CONTRACTOR TO REFER TO "C" DRAWINGS FOR ALL NECESSARY DRAINAGE DIMENSIONS. SITE PLANS ARE FOR GUIDANCE AND SHOWN LOCATED NOTES.
9. SEE "C" DRAWINGS FOR FINISH GRADE ELEVATIONS.
10. CONCRETE REINFORCING TO BE A MINIMUM OF 4" THICK W/ SPOILED JOINTS AT 8" O.C. DIMENSIONS/SPACING/DEPTH SHALL BE A MINIMUM 12" DIA. W/ 120 MAG. SLOPE EXTERIOR JOINTS TO HAVE ONE CORNER REINFORCING FELLOW MATERIAL OF 1/4". SEE "C" DRAWINGS FOR FINISH.
11. PAINT CURBS AND PROVIDE SIGNS TO MARK OFF FIVE LINES AS REQUIRED BY FIRE DEPARTMENT.
12. LANDSCAPING AREAS SHALL BE DELINEATED WITH A MINIMUM SIX INCHES (6") HIGH CURB.
13. APPROVED CONCEPTUAL LANDSCAPE PLAN PRIOR TO GRADING PERMIT.

SITE LEGEND

- LANDSCAPED AREA
- AC PARKING - SEE "C" DRAWING FOR DIMENSIONS
- CONCRETE DRIVE - SEE "C" DRAWING FOR DIMENSIONS
- EXTERIOR PARKING STRIP
- STANDARD PARKING STALL (8' x 18')
- HANDICAP PARKING STALL (8' x 18')
- PATH OF TRAVEL

HPANEVL000-09.jpg

lpa, Inc.
18071 Sandstone Way - Ste.
8100 Irvine, CA
92618
Tel: 949-450-1772
Fax: 949-450-1601
email: lpa@lpaarch.com

Owner:

Project:

Consultants:

Title: overall site plan

Project Number:

Drawn by:

Date:

Revised:

Sheet:

F-A1.1



HPA Inc.
16281 Redwood Avenue - Ste.
#100 Irvine, CA
92612
Tel: 949-453-1770
Fax: 949-453-0851
Web: hpa@hpaarch.com

Owner:

Project:

Consultants:

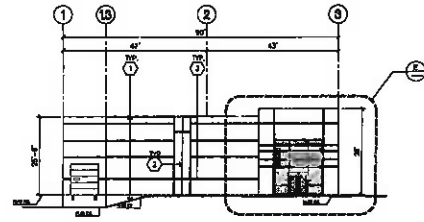
- Arch: _____
- Structural: _____
- Mechanical: _____
- Plumbing: _____
- Electrical: _____
- Landscaping: _____
- Fire Protection: _____
- Soft Engineer: _____

Title: Elevation

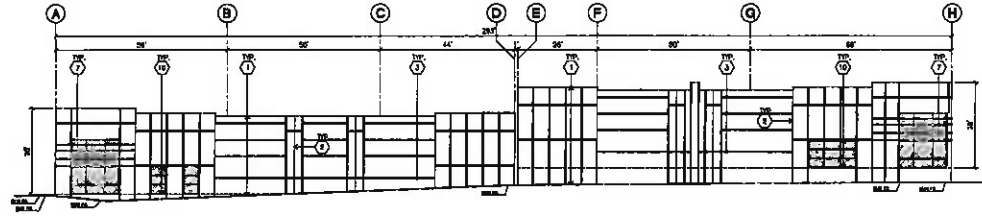
Project Number: _____
Drawn by: _____
Date: _____
Revisions: _____

Sheet:

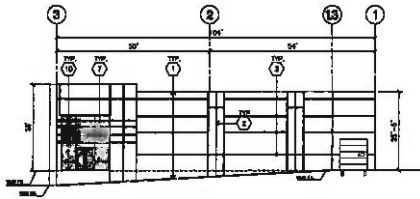
A3.1



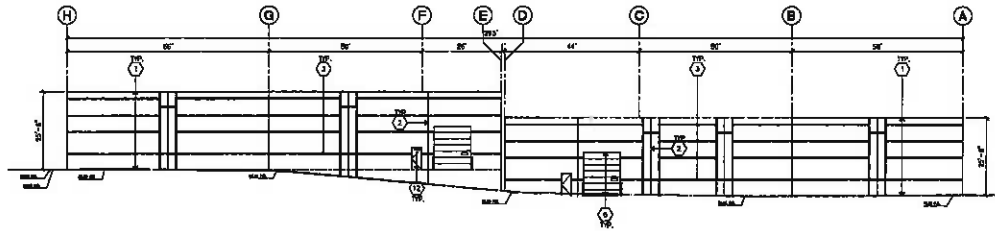
NORTH ELEVATION
SCALE: 1/8" = 1'-0"



WEST ELEVATION
SCALE: 1/8" = 1'-0"



SOUTH ELEVATION
SCALE: 1/8" = 1'-0"



EAST ELEVATION
SCALE: 1/8" = 1'-0"

ELEVATION KEYNOTES

- ① CONCRETE TILT-UP PANEL (PAINTED)
- ② PANEL JOINT
- ③ PANEL ROOF
- ④ METAL LINER
- ⑤ 3'-0" W X 10" H OVERHEAD DOOR @ DOCK HIGH
- ⑥ 12" W X 18" H OVERHEAD DOOR @ DRIVE THRU
- ⑦ METALLIC TUBE STEEL ALUMINUM PROFILING
- ⑧ METAL PANEL SYSTEM
- ⑨ CONCRETE STAIR, LANDING AND CONC. GUARDRAIL
- ⑩ DOCK SLAB
- ⑪ ALUMINUM STIFFENING FRAMING BY TYPICAL GLAZING AT ALL DOORS, SQUARES ADJACENT TO DOORS AND GLAZING W/ SQUARE LESS THAN 18" ABOVE F.F. ELEVATION
- ⑫ FINISH RETURN
- ⑬ HOLLOW METAL DOOR
- ⑭ INTERIOR CORNERPOST AND 2 OVERFLOW SCRAPERS
- ⑮ INTERIOR CORNERPOST AND 2 OVERFLOW SCRAPERS

GLAZING LEGEND

- WIND GLASS
- FINISHED GLASS

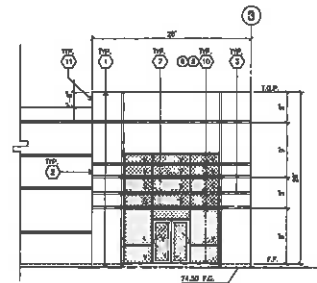
ALL GLAZING TO BE FINISHED
GLAZING FOR WINDOWS, DOORS AND BALCONIES TO BE 20' MIN
12' HIGHER GLASS UNIT WITH 1/2" UPRAISE AND 21' 1/2" UPRAISE
WALLS AND INTERIOR SLAB

ELEVATION GENERAL NOTES

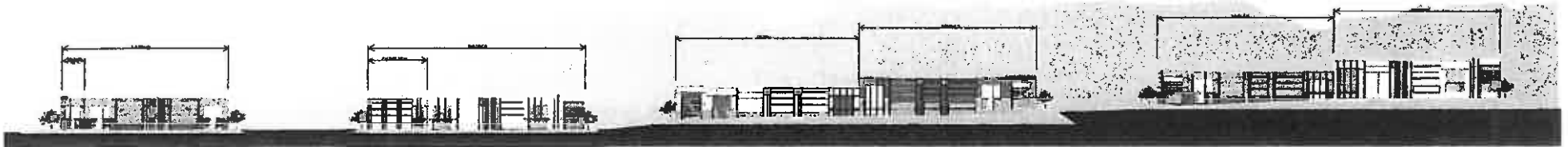
- A. ALL PAINT COLORS CHANGED TO OCCUR AT WORK CORNERS UNLESS NOTED OTHERWISE.
- B. ALL PAINT FINISHES ARE TO BE FLAT UNLESS NOTED OTHERWISE.
- C. T.O.P. = TOP OF FINISHER - ELEVATION.
- D. F.F. = FINISH FLOOR ELEVATION.
- E. STIFFENING CONSTRUCTION, GLASS, METAL ATTACHMENTS AND LANTERS TO BE DESIGNED TO RESIST TO WIND EXPOSURE. 1" WIND. CONTRACTOR TO SUBMIT SHOP DRAWINGS PRIOR TO METALWORK.
- F. CONTRACTOR SHALL PAINT ALL CONCRETE PANELS W/ SELECTED COLOR. ARCHITECT AND OWNER SHALL APPROVE PRIOR TO PAINTING REPAIRS OF BUILDING.

ELEVATION COLOR SCHEDULE

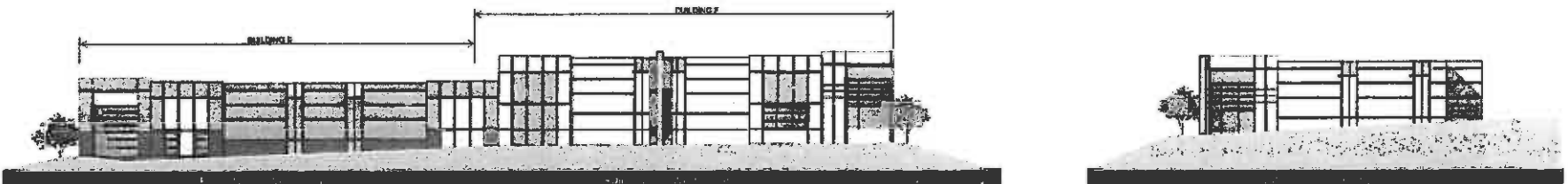
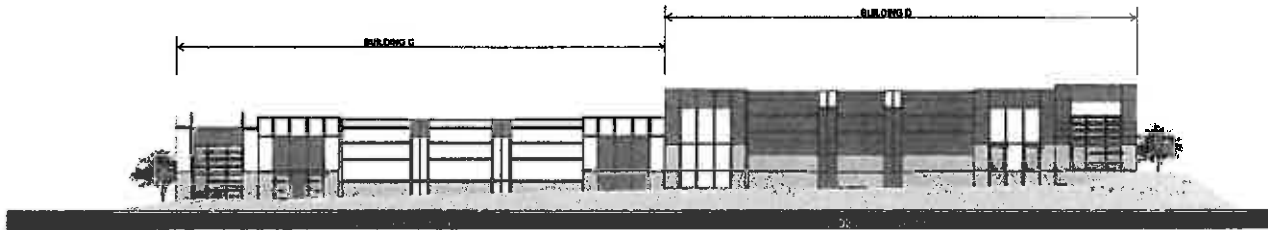
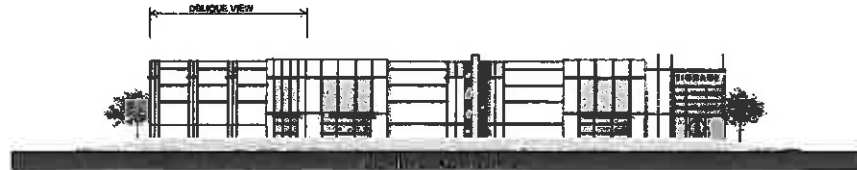
①	CONC. TILT-UP PANEL PAINT	_____
②	CONC. TILT-UP PANEL PAINT	_____
③	CONC. TILT-UP PANEL PAINT	_____
④	CONC. TILT-UP PANEL PAINT	_____
⑤	REFLECTIONS	COLOR CLEAR ANODIZED
⑥	GLAZING	COLOR BLUE REFLECTIVE
⑦	GLAZING	COLOR METAL CANOPY



ENLARGED NORTH ELEVATION
SCALE: 1/8" = 1'-0"



Street Elevation of Rustin Avenue (West Elevation)



RUSTIN AVE. & MARLBOROUGH AVE.

CITY OF RIVERSIDE, CA



PAGE BREAK



**AIRPORT LAND USE COMMISSION
RIVERSIDE COUNTY**



CHAIR
Simon Housman
Rancho Mirage

November 14, 2016

VICE CHAIRMAN
Rod Ballance
Riverside

Mr. Sean Kelleher, Project Planner
City of Riverside Community Development Department/Planning Division
3900 Main Street, Third Floor
Riverside CA 92522

COMMISSIONERS

Arthur Butler
Riverside

RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW

John Lyon
Riverside

File No.: ZAP1227MA16
Related File Nos.: P16-0671 (Design Review), P16-0672 (condominium Map)

Glen Holmes
Hemet

APN: 280-270-024

Steve Manos
Lake Elsinore

Dear Mr. Kelleher:

Russell Betts
Desert Hot Springs

Under the delegation of the Riverside County Airport Land Use Commission (ALUC), pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed City of Riverside Case No. P16-0671 (Design Review), a proposal to construct a 23,290 square foot two story medical office building on a 1.62 acre parcel located at 18876 Van Buren Boulevard easterly of Prairie Way and westerly of Wood Road. The applicant is also proposing a tentative map for condominium purposes (P16-0672).

STAFF

Director
Ed Cooper

ALUC previously found consistent ZAP1117MA15 which reviewed a proposal to develop 11,738 square foot retail tire store with 8 service bays and a 2,200 square foot fast food restaurant with drive-thru on this same parcel.

John Guerin
Paul Rull
Barbara Santos

County Administrative Center
4080 Lemon St., 14th Floor
Riverside, CA 92501
(951) 955-5132

The site is located within Airport Compatibility Zone D of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone D in this AIA, land use intensity is not restricted.

www.rcaluc.org

The elevation of Runway 14-32 at March Air Reserve Base/Inland Port Airport at its northerly terminus is approximately 1,535 feet above mean sea level (1535 feet AMSL). At a distance of approximately 18,560 feet from the runway to the property line, any structures at this location with an elevation exceeding 1,720 feet AMSL would require Federal Aviation Administration (FAA) obstruction evaluation review. The site elevation is 1,636 feet AMSL, and the proposed building height is 40 feet, resulting in a maximum top point elevation of 1,676 feet AMSL. Therefore, FAA review for height/elevation reasons is not required.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, subject to the following conditions:

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky.
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area, including but not limited to, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, artificial marshes, incinerators, and fly ash disposal.
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached notice shall be provided to all potential purchasers of the property and tenants of the proposed buildings, and shall be recorded as a deed notice.
4. Any new detention or retention basins on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention/retention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
5. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.

If you have any questions, please contact Paul Rull, ALUC Urban Regional Planner IV, at (951) 955-6893, or John Guerin, ALUC Principal Planner, at (951) 955-0982.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Edward C. Cooper, Director

PR:JG

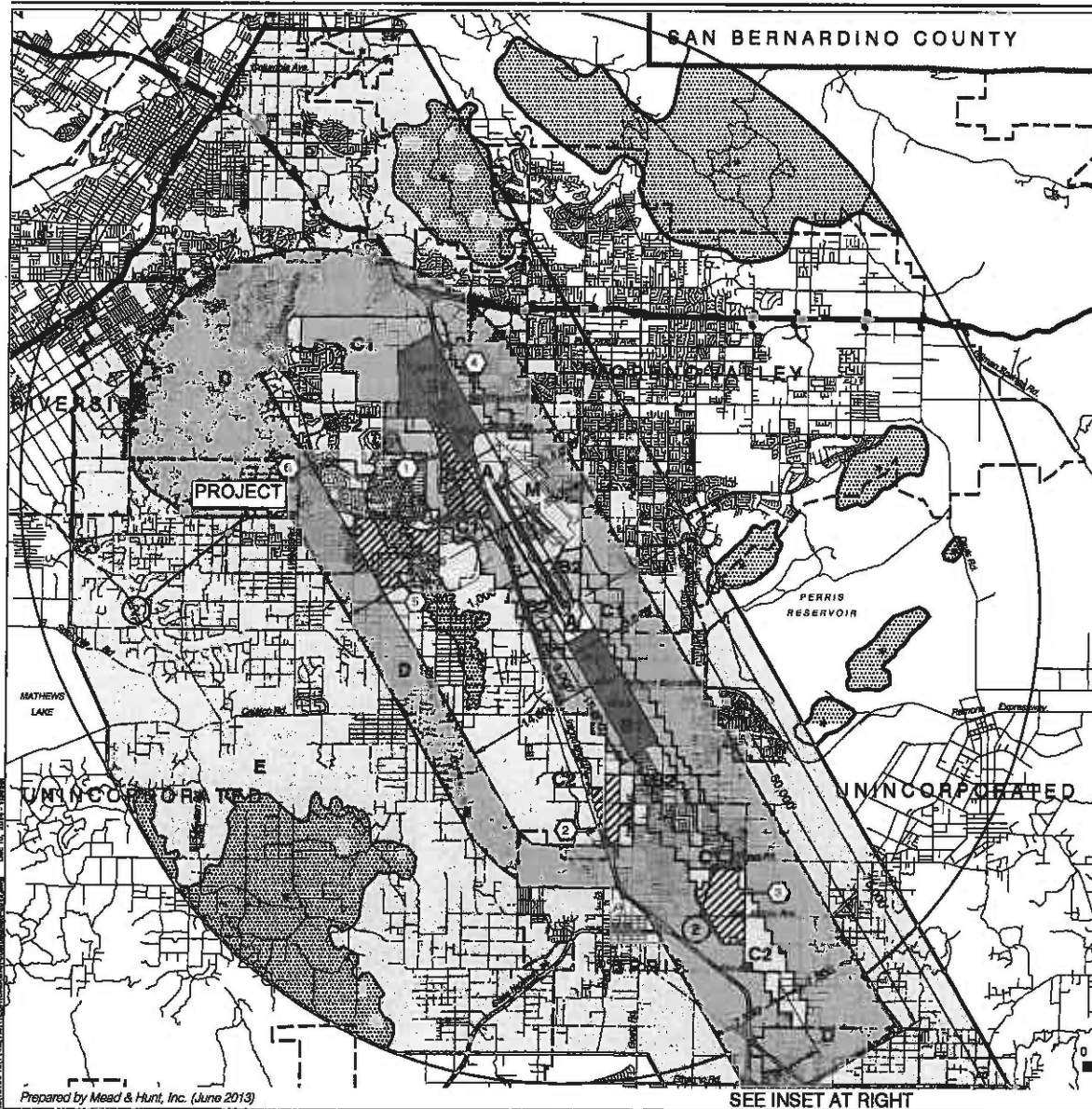
Attachments: Notice of Airport in Vicinity

cc: SS Commercial Properties, LLC (applicant/property owner)
Shakil Patel (representative/payee)
Gary Gosliga, Airport Manager, March Inland Port Airport Authority
Denise Hauser, March Air Reserve Base
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1227MA16\ZAP1227MA16.LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



LEGEND

Compatibility Zones

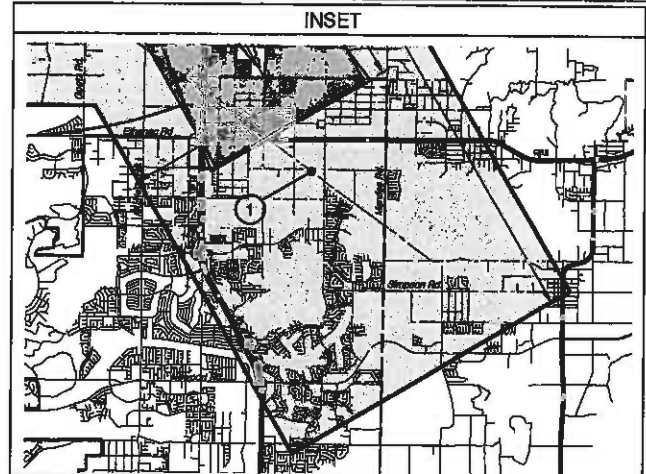
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

- 1 Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- 2 Point at which departing aircraft typically reach 3,000 feet above runway end.

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- ▨ Site-Specific Exceptions (existing local agency commitments to development projects)

- 1 March JPA: March Business Center/Meridian
- 2 Perris: Harvest Landing
- 3 Perris: Park West
- 4 Moreno Valley: Affordable Housing
- 5 March JPA: Ben Clark Training Center
- 6 Riverside: Ridge Crest Subdivision



**Riverside County
Airport Land Use Commission**

**March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan**

(Adopted November 13, 2014)

Map MA-1

Compatibility Map

March Air Reserve Base / Inland Port Airport

Note:
All dimensions are measured from
runway ends and centerlines.

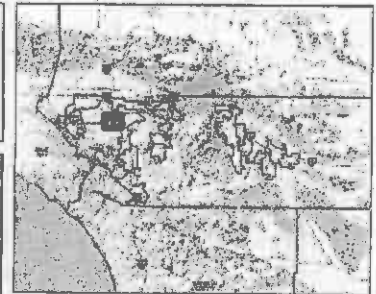


Base map source: County of Riverside 2013



























Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

My Map



Legend

-  Airports
-  AIA
- Airport Compatibility**
-  OTHER ZONE
-  A
-  A-EXC1
-  B1
-  B1-APZ I
-  B1-APZ I-EXC1
-  B1-APZ II
-  B1-APZ II-EXC1
-  B1-EXC1
-  B2
-  B2-EXC1
-  C
-  C1
-  C1-EXC1
-  C1-EXC3
-  C1-EXC4
-  C1-HIGHT
-  C2
-  C2-EXC1
-  C2-EXC2
-  C2-EXC3
-  C2-EXC5
-  C2-EXC6
-  C2-HIGHT



0 5,155 10,310 Feet



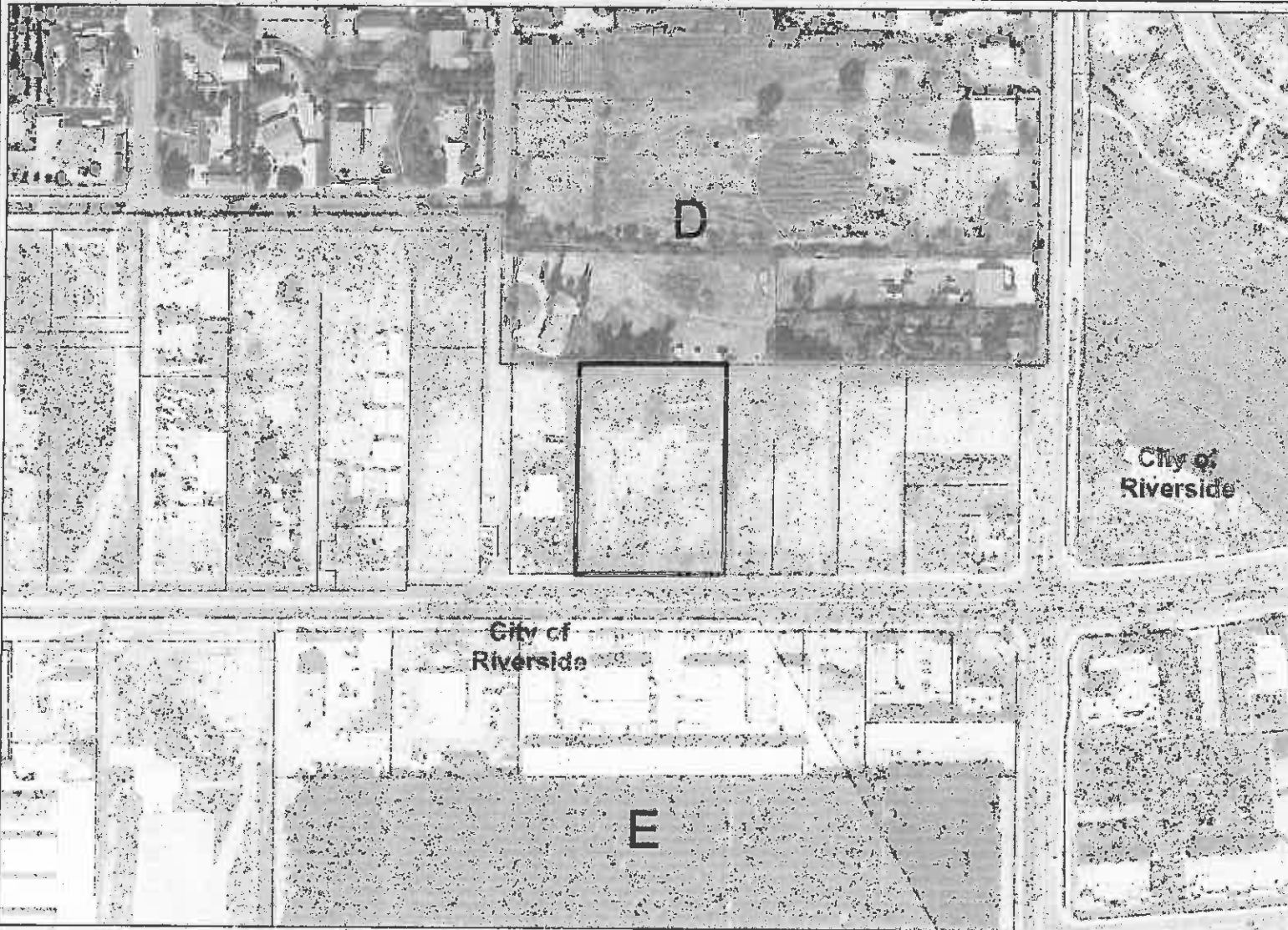
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/1/2016 3:20:14 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



0 322 644 Feet



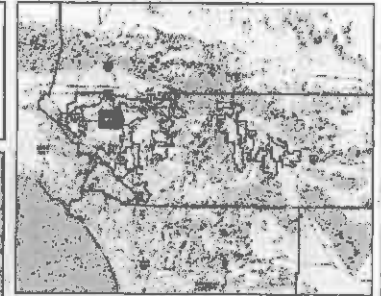
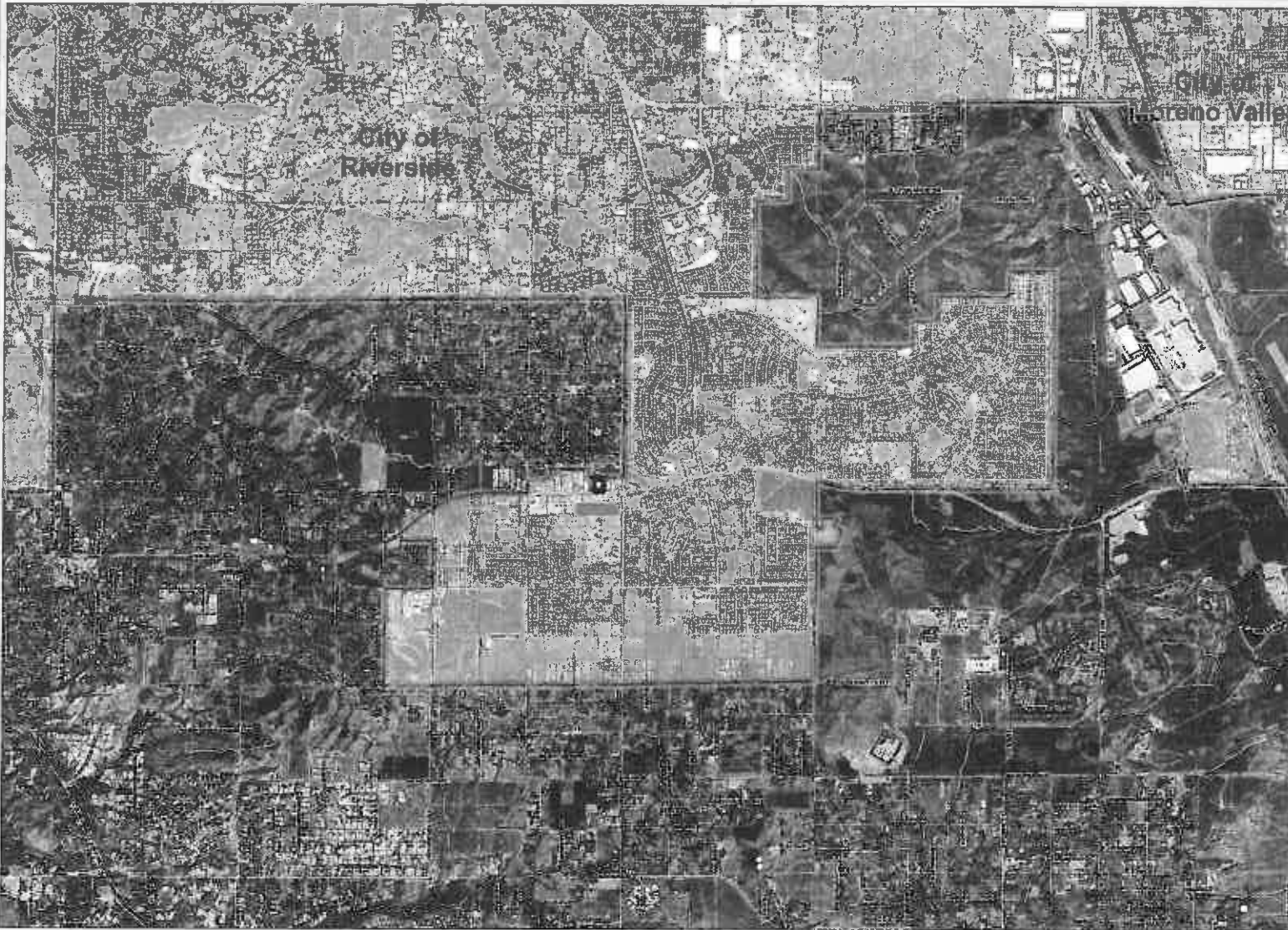
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/1/2016 3:17:55 PM











© Riverside County RCIT GIS

Notes

My Map



Legend

-  City Boundaries
- Cities
- highways_large
 -  HWY
 -  INTERCHANGE
 -  INTERSTATE
 -  USHWY
-  majorroads
-  counties
-  cities
- hydrographylines
- waterbodies
 -  Lakes
 -  Rivers



0 5,155 10,310 Feet



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/1/2016 3:21:06 PM






© Riverside County RCIT GIS

Notes

My Map



Legend

-  City Boundaries
- Cities**
- roads**
- anno**
- highways**
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads**
- Major Roads
- Arterial
- Collector
- Residential
-  counties
-  cities
- hydrography**
- lines**
- waterbodies**
-  Lakes
-  Rivers



0 2,578 5,155 Feet



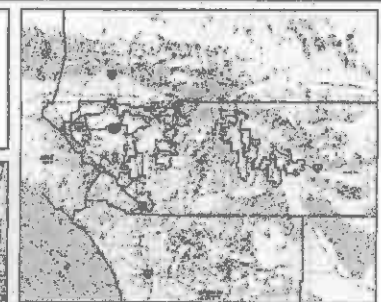
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/1/2016 3:21:58 PM






© Riverside County RCIT GIS

Notes

My Map



Legend

-  City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
-  counties
-  cities
- hydrographylines
- waterbodies
-  Lakes
-  Rivers



0 1,289 2,578 Feet



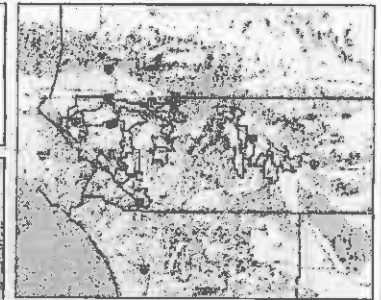
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/1/2016 3:22:37 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0

644

1,289 Feet



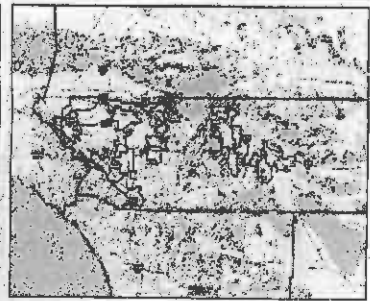
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/1/2016 3:23:18 PM






© Riverside County RCIT GIS

Notes

My Map



Legend

-  City Boundaries
- Cities**
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
-  counties
-  cities
- hydrographyllines
- waterbodies
-  Lakes
-  Rivers



0 322 644 Feet



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/1/2016 3:18:49 PM












© Riverside County RCIT GIS

Notes

My Map



Legend

-  City Boundaries
- Cities
- roadsanno
- highways
 -  HWY
 -  INTERCHANGE
 -  INTERSTATE
 -  OFFRAMP
 -  ONRAMP
 -  USHWY
-  counties
-  cities
- hydrographylines
- waterbodies
 -  Lakes
 -  Rivers



0 161 322 Feet



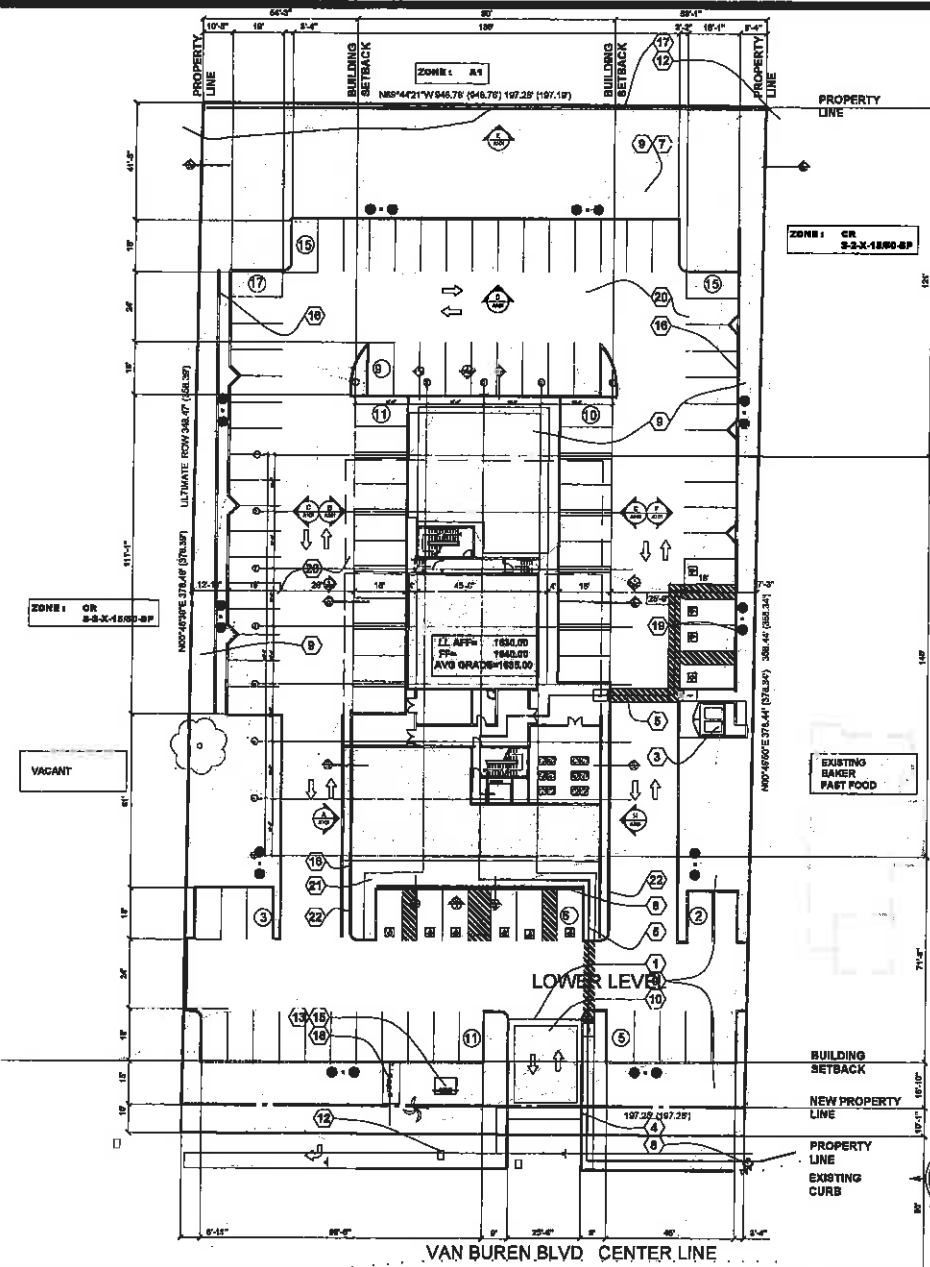
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 11/1/2016 3:24:34 PM

© Riverside County RCIT GIS

Notes

C:\Users\lsana\Desktop\projects\SIMJEE MP\PLANNING\2 SUBV2 DRC\SP1.dwg, 11/9/2016 12:28:12 PM, DWG To PDF.p3



SITE PLAN

OWNER
 55 COMMERCIAL PROPERTY LLC
 8502 E CHAPMAN #412, ORANGE CA
APPLICANT
 SHAKIL PATEL AND ASSOCIATES AIA
 28482 HINCKLEY STREET, LOMA LINDA CA 92354
 TELEPHONE: 904-796-0900 patelshakile@gmail.com

SITE
 APN 280-210-024
 LOT AREA 10,400 SF 1620 ACRES.
 ADDRESS 18876 VAN BUREN BLVD RIVERSIDE CA.

PLANNING
 ZONE CR5-2-X-15/50-SP
BUILDING SETBACK:
 FRONT 15'-0"
 REAR 50'-0"
 SIDE 0'-0"
 LOT COVERAGE 50 %
 HEIGHT 30'-0"

LANDSCAPE
 LANDSCAPE PARKING = 5 %

UTILITIES
 SEWER CITY OF RIVERSIDE
 WATER CITY OF RIVERSIDE
 ELECTRICAL CITY OF RIVERSIDE
 GAS SCG
 SCHOOL DIST. RIVERSIDE USD

PROJECT DATA

- PROPOSED CONCRETE DRIVEWAY
- INDICATES 24 IN WIDE FIRE TRUCK ROUTE
- TRASH ENCLOSURE PER CITY OF RIVERSIDE STD.
- ADA RAMP AT DRIVEWAY SEE PRELIM GRADING PLAN
- ADA PATH OF TRAVEL
- ZERO CURB ADA STALLS TYP.
- WATER DETENTION BASIN
- EXISTING PUBLIC FIRE HYDRANT
- LANDSCAPED AREA, TYP.
- COLORLED STAMPED CONCRETE
- POC OF SEWER AT WOODS STREET
- POC WATER AT VAN BUREN BLVD
- POC ELECTRICAL AT VAN BUREN BLVD
- RIVERSIDE UTILITIES UNDER GROUND VAULT 8X10 FT 8FT CLEARANCE IN FRONT AND 3 FT ALL AROUND.
- CMU RETAINING WALL SEE PRELIMINARY GRADING PLAN
- 6'-0" HIGH CMU PROPERTY WALL AND ADO
- FIRE DEPT. "DOG" CONNECTION SEE WATER AND UNDERGROUND FIRE SPRINKLER DRAWINGS
- POLE MOUNTED ACCENT LIGHT FIXTURE TYP.
- AG PAVING SEE SOIL REPORT
- FOUR (4) BICYCLE STAND SEE DETAIL 18/AD2
- 22.42' HIGH GUARD RAIL TYP.

KEY NOTES



VICINITY MAP

SHAKIL PATEL & ASSOCIATES AIA, LEED AP
 28482 HINCKLEY STREET, LOMA LINDA, CALIFORNIA
 904-796-0900
 904-796-0900
 Architecture • Interiors • Construction Management

SIMJEE MEDICAL PLAZA
 18876 VAN BUREN BLVD RIVERSIDE, CA
 APN 280-210-011812

BUILDING AREA TABULATION

GROSS AREA:

LOWER LEVEL	2305.00 SF
MAIN TOTAL	42,800.00 SF
UPPER TOTAL	9,900.00 SF
TOTAL	23,950.00 SF

LEASEABLE AREA

MAIN TOTAL	10,915.00 SF
UPPER TOTAL	7,758.00 SF
TOTAL	18,270.00 SF

LOWER LEVEL 2305.00 SF

COMMON AREA:

MAIN LEVEL	2,065.00 SF
UPPER LEVEL	1,235.00 SF
TOTAL	3,310.00 SF

PARKING CALCULATION
 MED OFF.
 18270 / 160 = 102 STALLS

TOTAL STALLS = 108 STALLS

TOTAL AREA 23,950.00 SF

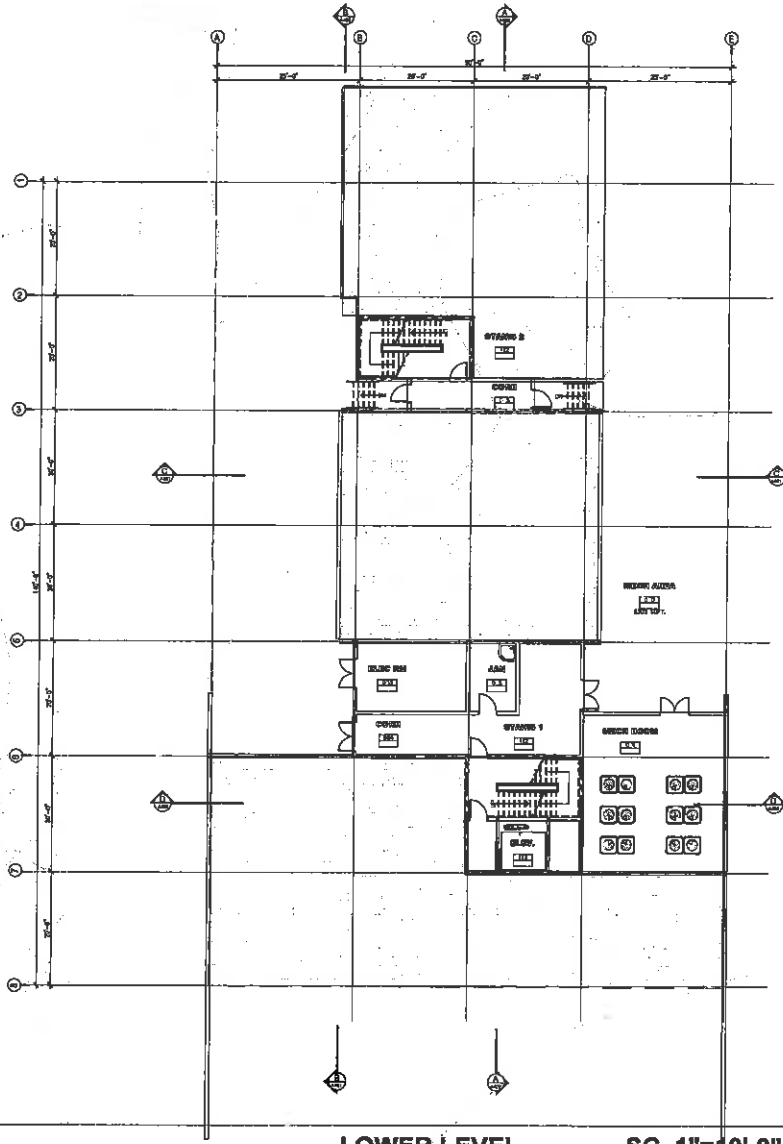
ACK STALLS = 18 STALLS @ 90%
 REGULAR 177.5' X 100' 94 STALLS

DEVELOPMENT REVIEW
 P16-0671 (DR)
 P16-0672 (PM)
 P18-0673 (VR)

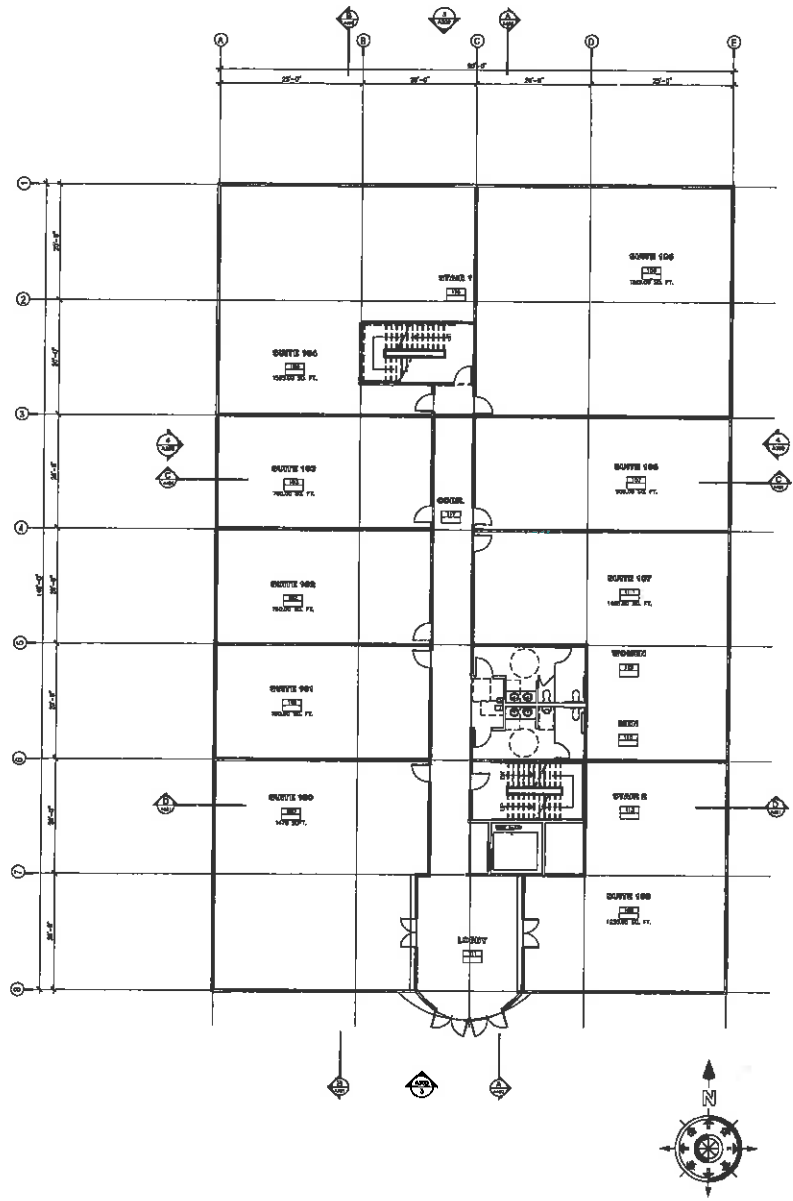
DATE	11/9/2016
BY	LSANA
SCALE	AS SHOWN
PROJECT	SIMJEE MEDICAL PLAZA
SHEET NO.	SP
TOTAL SHEETS	SP
DATE	11/9/2016

SITE PLAN

C:\Users\sana\Desktop\projects\SIMJEE MP\PLANNING\2 SUBVP.dwg, 11/9/2016 12:25:22 PM, DWG To PDF.pc3



LOWER LEVEL SC 1"=10'-0"



MAIN LEVEL SC 1"=10'-0"

SHAMOL PATEL & ASSOCIATES INC. LEED AP
 2542 HICKLEY STREET, LANALINDA, CALIFORNIA 92653
 Telephone: 949.276.6949 Fax: 949.276.4007
 Architects, Engineers, Construction Management

SIMJEE MEDICAL PLAZA.
 18675 VAN BUREN BLVD RIVERSIDE, CA
 APR 2015-2016

AREA TABULATION MAIN LEVEL

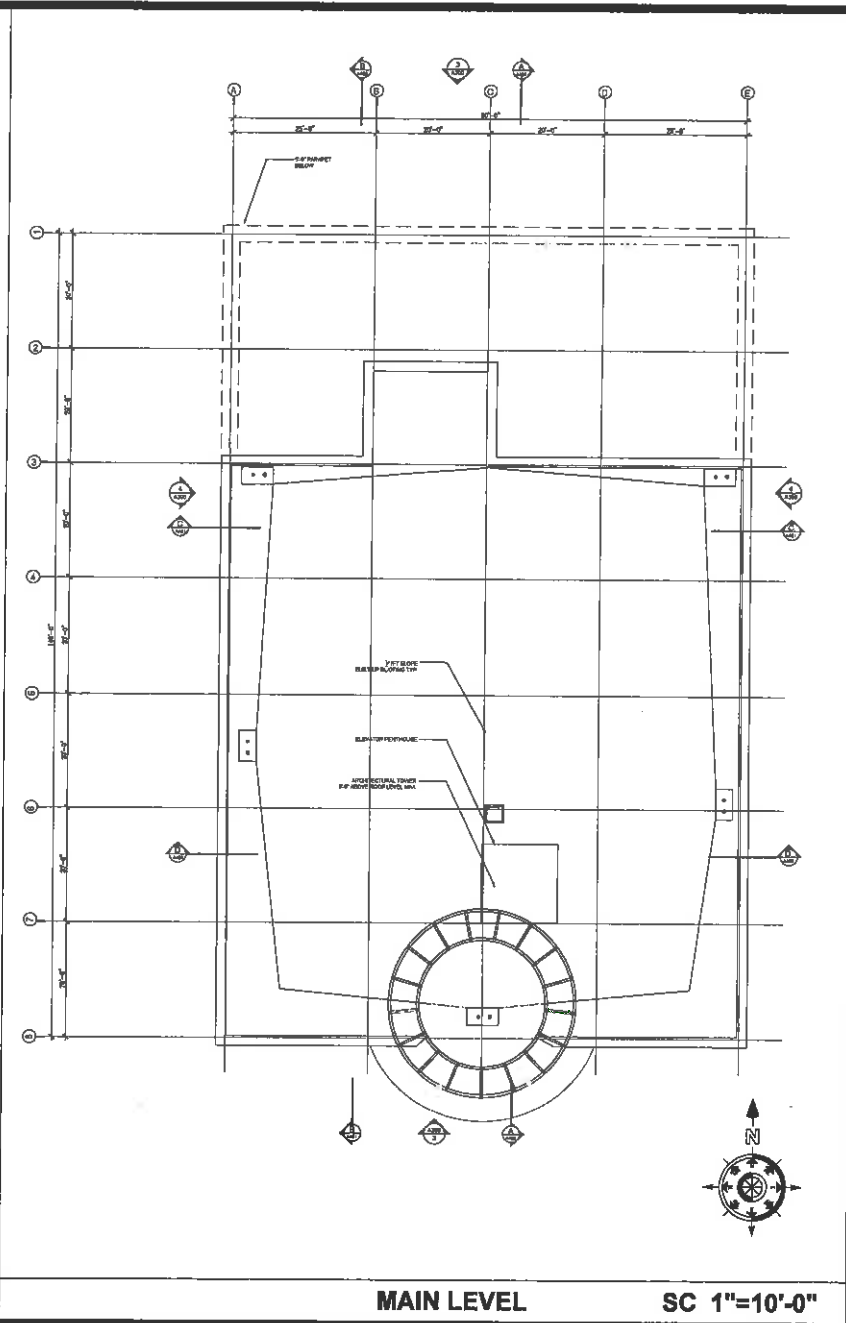
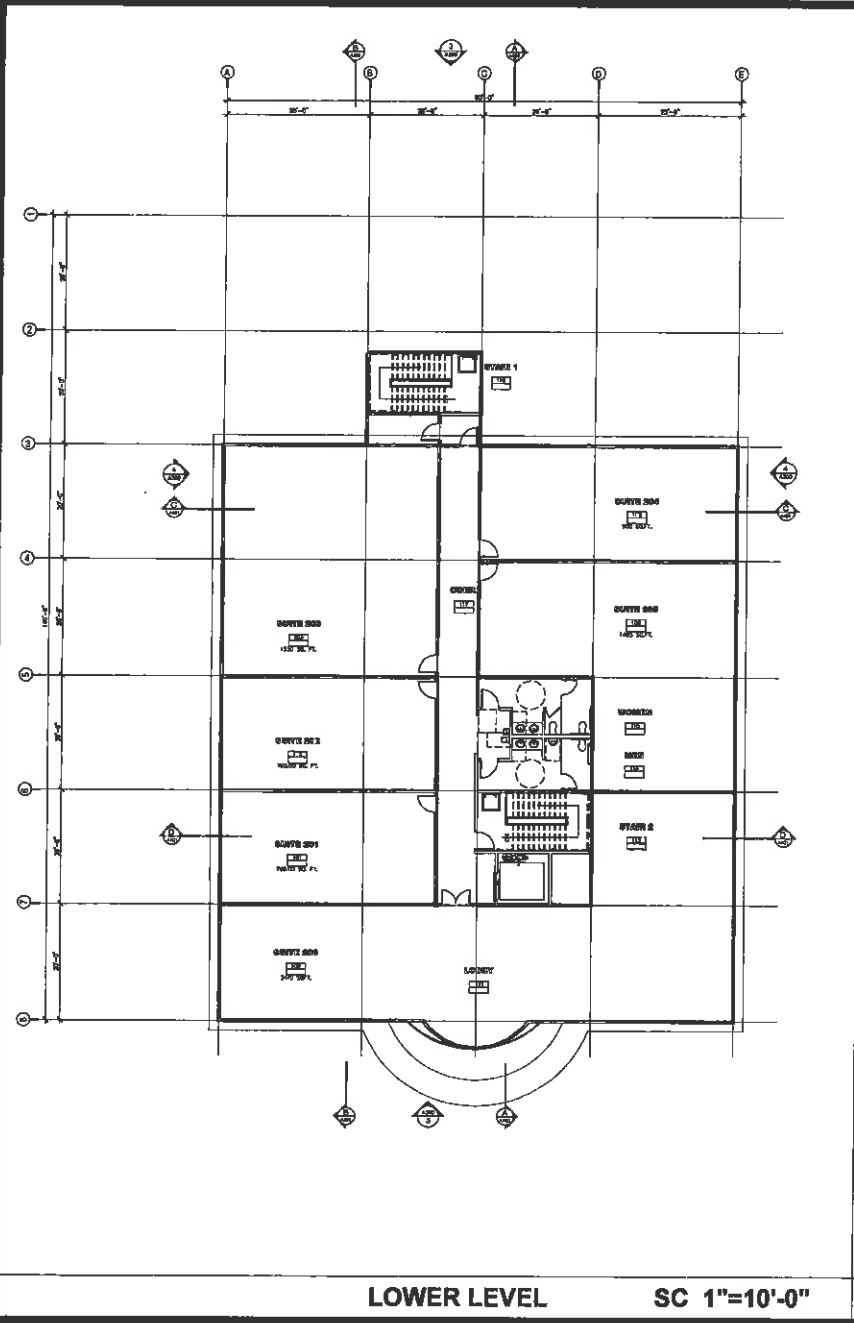
SUITE 100	1470.00 SF LEASEAB
SUITE 101	700.00 SF LEASEAB
SUITE 102	700.00 SF LEASEAB
SUITE 103	700.00 SF LEASEAB
SUITE 104	1509.00 SF LEASEAB
SUITE 105	1015.00 SF LEASEAB
SUITE 106	800.00 SF LEASEAB
SUITE 107	1405.00 SF LEASEAB
SUITE 108	1250.00 SF LEASEAB
TOTAL	10,515.00 SF
COMMON	2,085.00 SF
GROSS AREA	12,600.00 SF

DEVELOPMENT REVIEW
 P16-0671 (DR)
 P16-0672 (PM)
 P16-0673 (VR)

NO.	DATE	REVISION

PROJECT NO: 020113
 DRAWN BY: SP
 CHECKED BY: SP
 SHEET FILE:
FLOOR PLANS

C:\Users\Yama\Desktop\projects\SIMJEE MPA\PLANNING\2 SUB\FP.dwg, 11/9/2016 12:27:46 PM, DWG To PDF.pc3



SHAWI, PATEL & ASSOCIATES, INC. 15812 HICKLEY STREET, LAKELAND, CALIFORNIA 91301
 Architects, Engineers, Construction Management

CONSENTS

OWNER

SIMJEE MEDICAL PLAZA
 1876 VAN BUREN BLVD RIVERSIDE, CA
 APN 380-270-011012

AREA TABULATION UPPER LEVEL

SUITE 200	2410.00 SF LEASEABLE
SUITE 201	760.00 SF LEASEABLE
SUITE 202	760.00 SF LEASEABLE
SUITE 203	1330.00 SF LEASEABLE
SUITE 204	800.00 SF LEASEABLE
SUITE 205	1405.00 SF LEASEABLE
TOTAL	7,765.00 SF
COMMON	1,235.00 SF
GROSS AREA	9,000.00 SF

DEVELOPMENT REVIEW
 P16-0671 (DR)
 P16-0672 (PM)
 P16-0673 (VR)

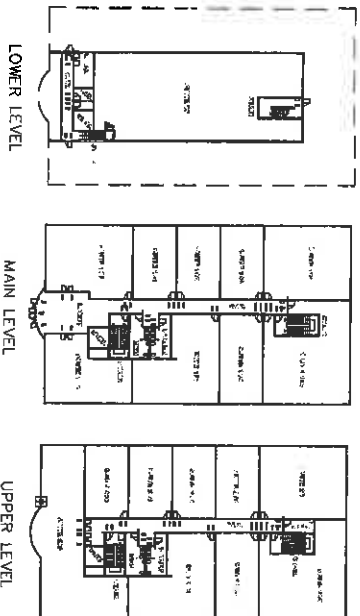
DATE	NO.	DESCRIPTION

PROJECT NO. 22013
 DRAWN BY: SP
 CHECKED BY: SP
 SHEET TITLE:

FLOOR PLANS

TENTATIVE TRACT MAP NO. 37218

FOR CONDOMINIUM PURPOSES



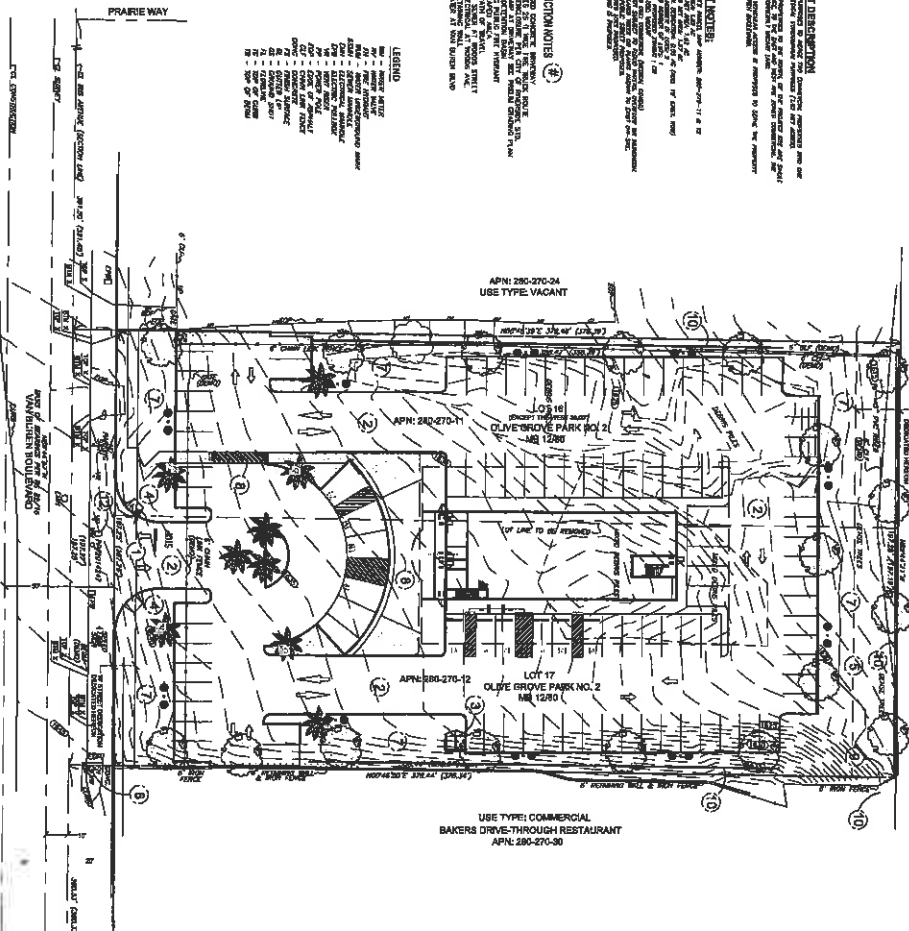
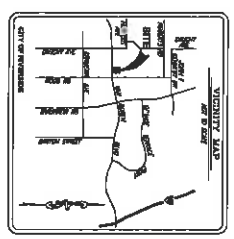
AREA TABULATION	
AREA LEVEL	
NET AREA	8,990.00 SF
COMMON AREA	11,200.00 SF
TOTAL	20,190.00 SF
COMMON AREA	11,200.00 SF
TOTAL	10,200.00 SF
COMMON AREA	1,000.00 SF
TOTAL	11,200.00 SF

BUILDING AREA TABULATION	
LOWER LEVEL	890.00 SF
MAIN LEVEL	1,700.00 SF
UPPER LEVEL	1,000.00 SF
TOTAL	3,590.00 SF
COMMON AREA	1,700.00 SF
TOTAL	5,290.00 SF

LEGAL DESCRIPTION
 THE TRACT OF LAND DESCRIBED AS BEING A PORTION OF THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:
 BEING THAT PORTION OF THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:
 COMMENCE AT THE POINT OF BEGINNING OF THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:

UTILITIES
 THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:
 THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:
 THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:

PARKING CALCULATION
 PARK OFF-
 ROAD STALLS = 53 STALLS
 TOTAL STALLS = 54 STALLS
 TOTAL AREA = 22,400.00 SF
 MIN. STALLS = 52 STALLS MIN.
 MIN. AREA = 22,400.00 SF



PROJECT DESCRIPTION
 THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:
 THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:
 THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:

CONSTRUCTION NOTES
 THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:
 THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:
 THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:

LEGEND
 THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:
 THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:
 THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:

BENCH MARK
 THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:
 THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:
 THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:

BASES OF BEHAVIOR
 THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:
 THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:
 THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:

SCALE
 THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:
 THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:
 THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:

DATE
 THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:
 THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:
 THE TRACT OF LAND BEING TRACT MAP NO. 37218, AS SHOWN ON THE TENTATIVE TRACT MAP NO. 37218, FOR CONDOMINIUM PURPOSES, AND IS DESCRIBED AS FOLLOWS:

		Prepared Under the Supervision of [Name] [Title]	TENTATIVE TRACT MAP 280-270-08, 280-270-09, 280-270-11, 280-270-12 WOOD ROAD, CALIFORNIA [Address]	Date: 1/1/2024
--	--	--	---	----------------

PAGE BREAK



AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY



CHAIR November 3, 2016

Simon Housman
Rancho Mirage

VICE CHAIRMAN
Rod Ballance
Riverside

COMMISSIONERS

Arthur Butler
Riverside

John Lyon
Riverside

Glen Holmes
Hemet

Steve Manos
Lake Eisnora

Russell Betts
Desert Hot Springs

Ms. Dionne Harris, Project Planner
Riverside County Planning Department
4080 Lemon Street, 12th Floor
Riverside CA 92501
(VIA HAND DELIVERY)

RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW – DIRECTOR'S DETERMINATION

File No.: ZAP1219MA16 – Letter 1 of 2
Related File No.: GPA01183 (General Plan Amendment), CZ07919 (Change of Zone)
APNs: 309-060-001, 309-060-004

Dear Ms. Harris:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to ALUC Resolution No. 2015-01 (as adopted on August 13, 2015), staff reviewed County of Riverside Case Nos. GPA01183 (General Plan Amendment), a proposal to amend the General Plan (Lakeview/Nuevo Area Plan) land use designation of an 18.36-acre area consisting of the above-referenced parcels located southerly of Central Avenue, northwesterly of Porter Street, and easterly of Palomar Road in the unincorporated community of Lakeview/Nuevo from Rural Community: Low Density Residential (RC-LDR) to Community Development: Medium Density Residential, and CZ07919 (Change of Zone), a proposal to change the zoning of these two parcels from Residential Agricultural (R-A) to Planned Residential (R-4).

The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, residential density is not restricted.

As ALUC Director, I hereby find the above-referenced projects **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan.

This finding of consistency relates to airport compatibility issues and does not necessarily constitute an endorsement of the proposed actions. As the site is located within Airport Compatibility Zone E, both the existing and the proposed General Plan land use designations and zoning are consistent with the March ALUCP.

STAFF

Director
Ed Cooper

John Guerin
Paul Rull
Barbara Santos

County Administrative Center
4080 Lemon St., 14th Floor
Riverside, CA 92501
(951) 955-6132

www.rcaluc.org

AIRPORT LAND USE COMMISSION

If you have any questions, please contact Paul Rull, ALUC Urban Regional Planner IV, at (951) 955-6893 or John Guerin, ALUC Principal Planner, at (951) 955-0982.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Edward C. Cooper, Director

cc: Richard Marcus (applicant/property owner/payee)
Oliver Cagle (representative)
Gary Gosliga, Airport Manager, March Inland Port Airport Authority
Denise Hauser, March Air Reserve Base
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1219MA16\ZAP1219MA16.LTR1.doc

**AIRPORT LAND USE COMMISSION
RIVERSIDE COUNTY**



CHAIR
Simon Housman
Rancho Mirage

VICE CHAIRMAN
Rod Ballance
Riverside

COMMISSIONERS

Arthur Butler
Riverside

John Lyon
Riverside

Glen Holmes
Hemet

Steve Manos
Lake Elsinore

Russell Betts
Desert Hot Springs

November 3, 2016

Ms. Dionne Harris, Project Planner
Riverside County Planning Department
4080 Lemon Street, 12th Floor
Riverside CA 92501
(VIA HAND DELIVERY)

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –
DIRECTOR’S DETERMINATION**

File No.: ZAP1219MA16 – Letter 2 of 2
Related File No.: TR37134 (Tentative Tract Map)
APNs: 309-060-001, 309-060-004

Dear Ms. Harris:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed County of Riverside Case No. TR37134 (Tentative Tract Map), a proposal to divide an 18.36-acre area consisting of the above-referenced parcels located southerly of Central Avenue, northwesterly of Porter Street, and easterly of Palomar Road in the unincorporated community of Lakeview/Nuevo into 75 single-family residential lots.

The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, residential density is not restricted.

The elevation of Runway 14-32 at March Air Reserve Base/Inland Port Airport at its southerly terminus is approximately 1,488 feet above mean sea level (AMSL). Given the project site elevation of 1,448 feet AMSL and the proposed maximum building height of 40 feet, structures here would not exceed the elevation of that runway. The site is actually closer to Perris Valley Airport (19,281 feet). The elevation of the runway at Perris Valley Airport is 1413 feet AMSL. At a distance of 19,281 feet from that runway to the project, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with a top of roof exceeding 1,605 feet AMSL. As the maximum top point elevation at this site would be 1488 feet AMSL, review by the FAA OES was not required.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, subject to the following conditions:

STAFF

Director
Ed Cooper

John Guerin
Paul Rull
Barbara Santos

County Administrative Center
4080 Lemon St, 14th Floor
Riverside, CA 92501
(951) 965-6132

www.rcaluc.org

AIRPORT LAND USE COMMISSION

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The review of this Tentative Tract Map is based on the proposed uses and activities noted in the project description. The following uses/activities are not included in the proposed project and shall be prohibited at this site, in accordance with Note A on Table 4 of the Lakeview/Nuevo Area Plan:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area.
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The landowner shall provide the attached notice to all potential purchasers of the proposed lots and to any tenants/lessees of the home(s) thereon.

If you have any questions, please contact Paul Rull, ALUC Urban Regional Planner IV, at (951) 955-6893 or John Guerin, ALUC Principal Planner, at (951) 955-0982.

Sincerely,

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Edward C. Cooper, Director

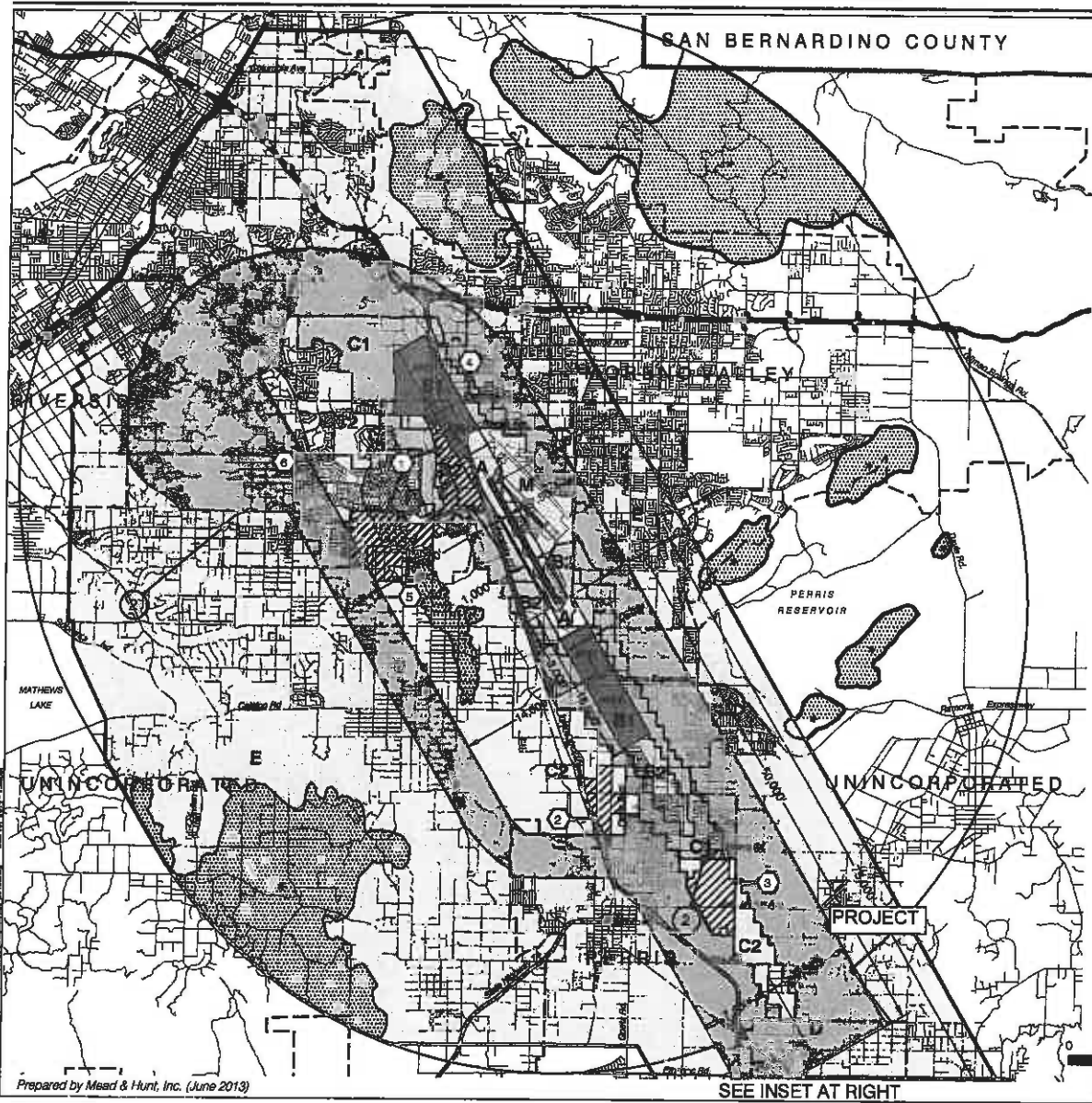
Attachments: Notice of Airport in Vicinity

cc: Richard Marcus (applicant/property owner/payee)
Oliver Cagle (representative)
Gary Gosliga, Airport Manager, March Inland Port Airport Authority
Denise Hauser, March Air Reserve Base
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1219MA16\ZAP1219MA16.LTR2.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



LEGEND

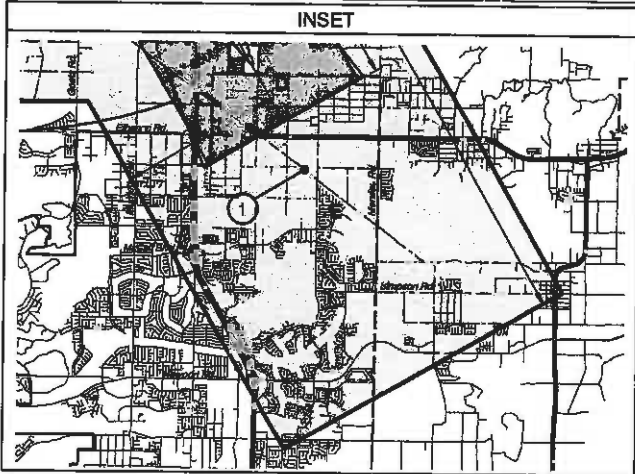
Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- ▨ Site-Specific Exceptions (existing local agency commitments to development projects)
- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



Note:
All dimensions are measured from runway ends and centerlines.



Base map source: County of Riverside 2013

Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)

Map MA-1

Compatibility Map
March Air Reserve Base / Inland Port Airport

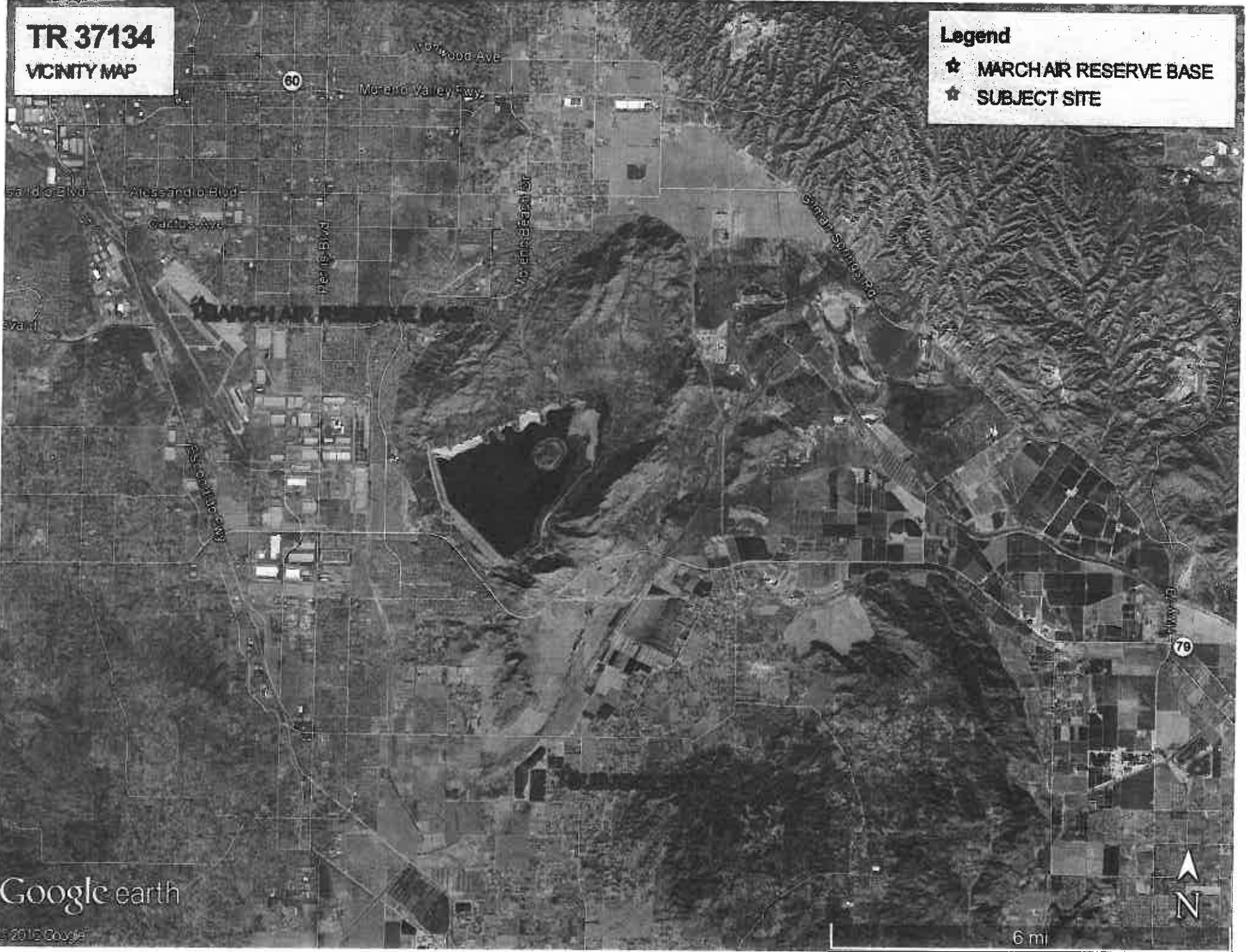
Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

TR 37134
VICINITY MAP

Legend

- ★ MARCH AIR RESERVE BASE
- ★ SUBJECT SITE



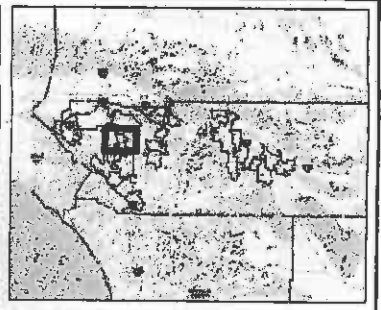
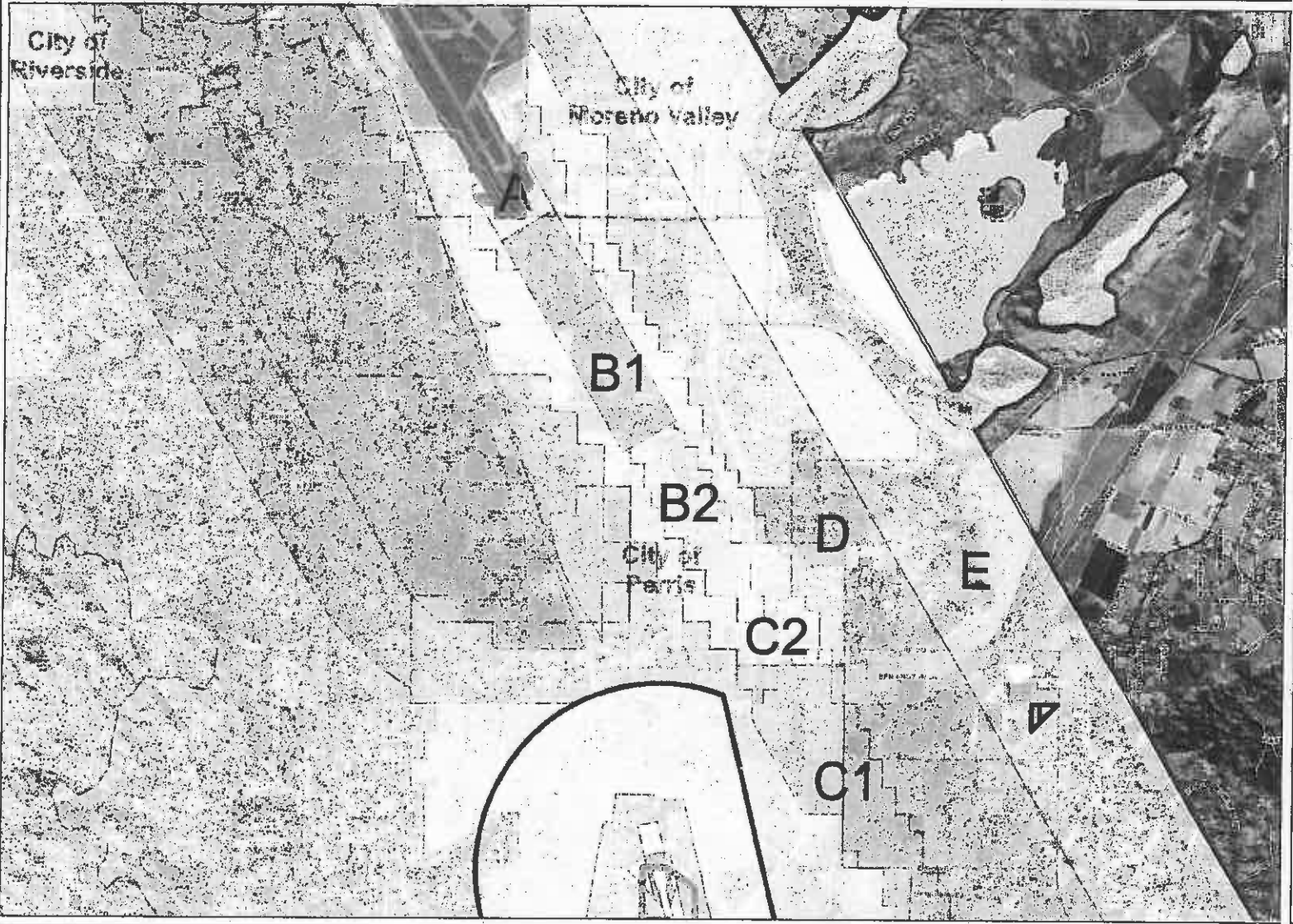
Google earth

© 2015 Google

6 mi



My Map



Legend

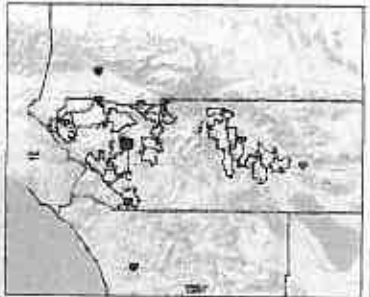
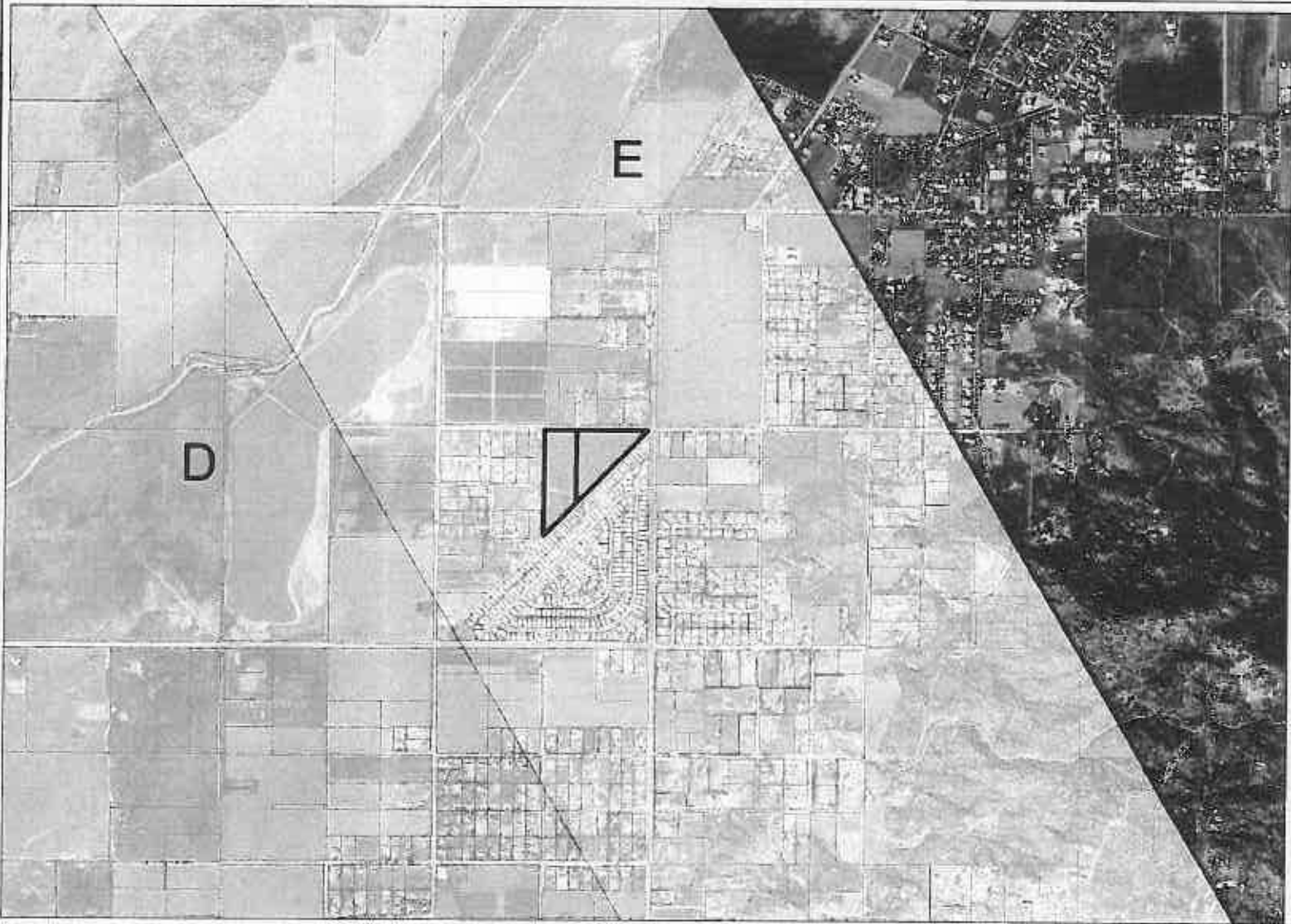
- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

Notes

My Map



Legend

- Airports
- AIA

Airport Compatibility

- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



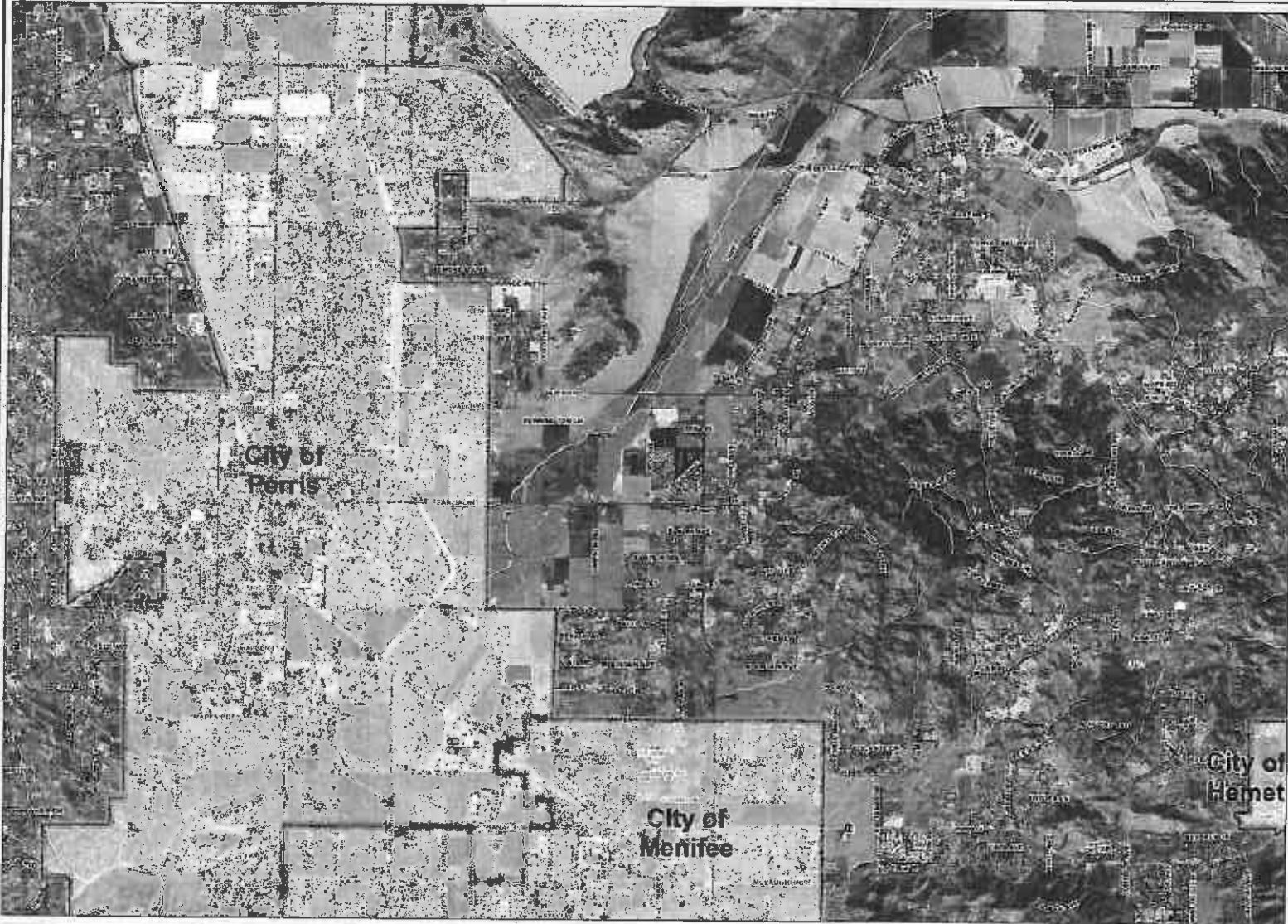
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/19/2016 3:27:29 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- City Boundaries
- Cities**
- adjacent_highways**
- Interstate
- Interstate 3
- State Highways; 60
- State Highways 3
- US HWY
- OUT
- highways_large**
- HWY
- INTERCHANGE
- INTERSTATE
- USHWY
- counties
- cities



0 9,040 18,080 Feet



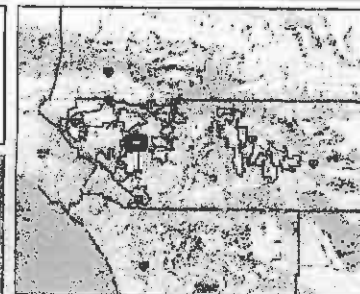
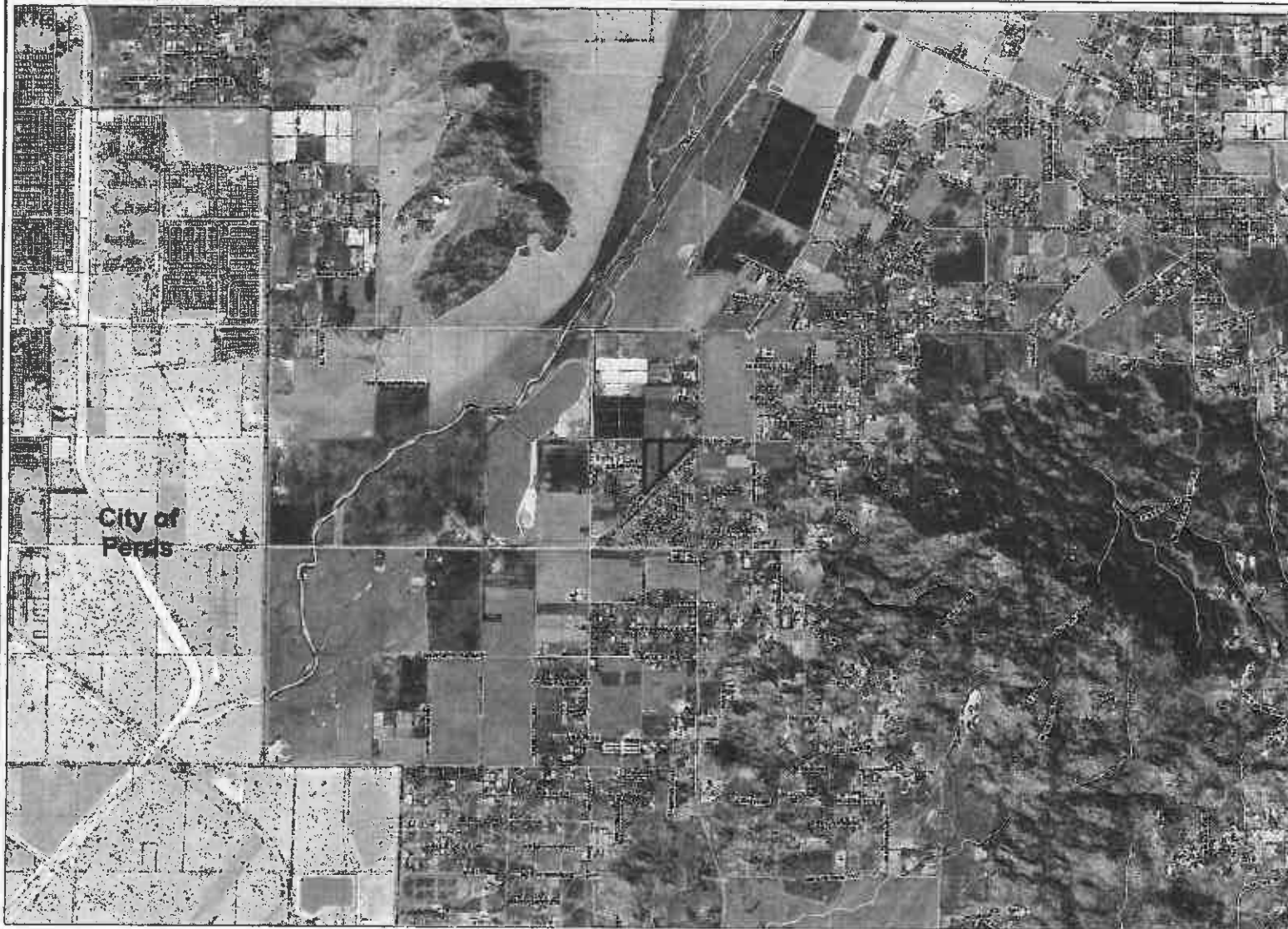
"IMPORTANT" Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/19/2016 3:31:21 PM













© Riverside County RCIT GIS

Notes

My Map



Legend

-  City Boundaries
- Cities**
- highways**
 -  HWY
 -  INTERCHANGE
 -  INTERSTATE
 -  OFFRAMP
 -  ONRAMP
 -  USHWY
-  majorroads
-  counties
-  cities
- hydrographylines**
- waterbodies**
 -  Lakes
 -  Rivers



0 4,520 9,040 Feet



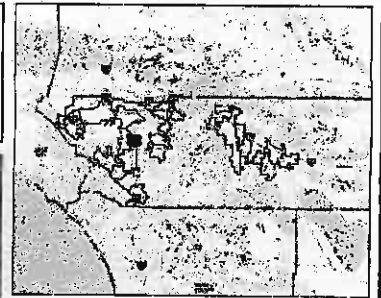
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/19/2016 3:31:52 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

0 2,260 4,520 Feet

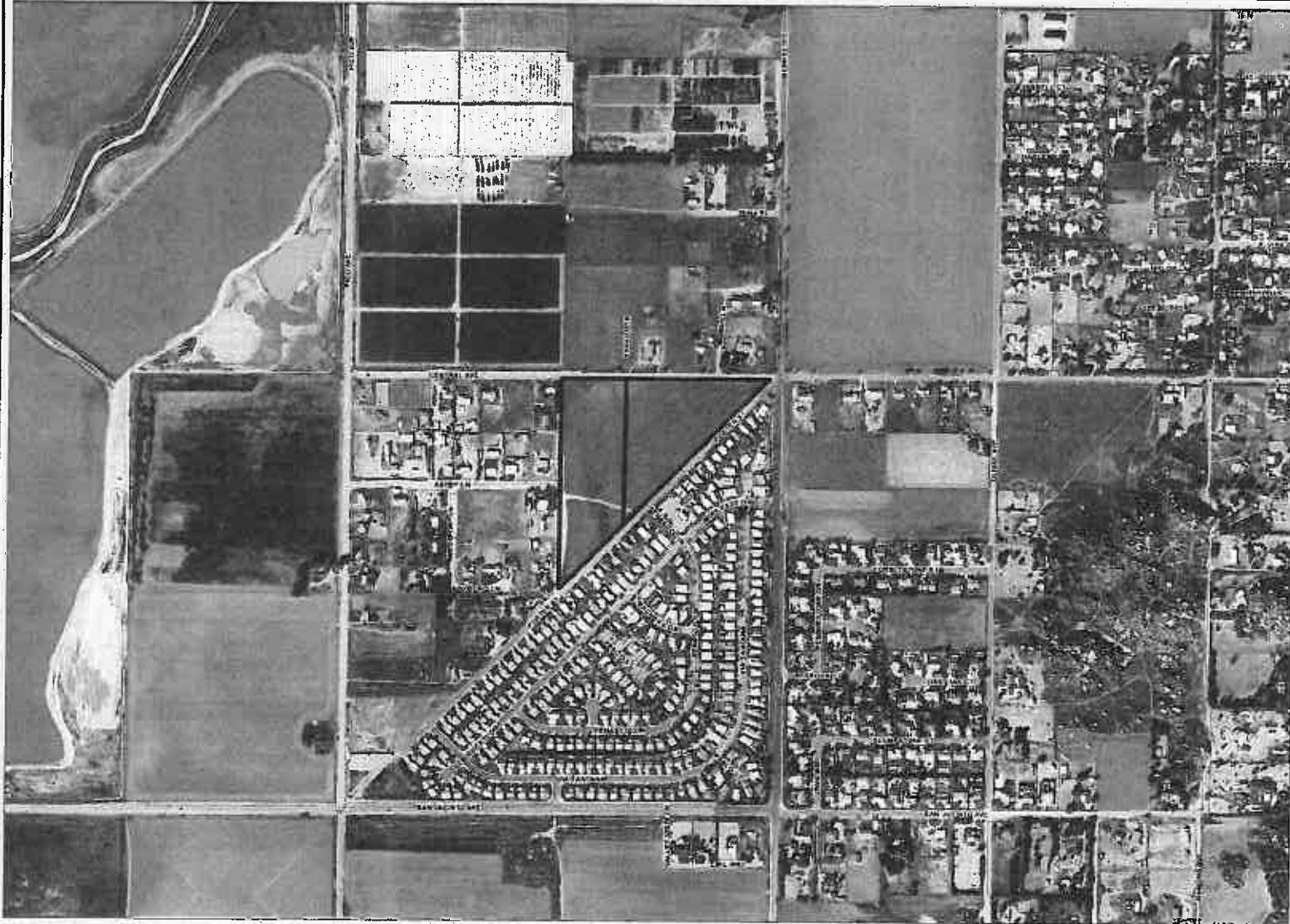


REPORT PRINTED ON... 10/19/2016 3:32:21 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrography/lines
- waterbodies
- Lakes
- Rivers



0 1,130 2,260 Feet



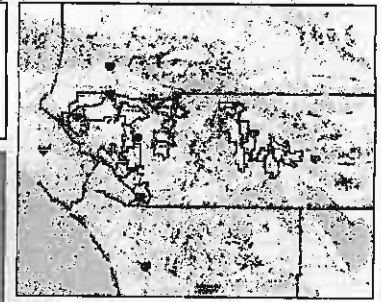
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/19/2016 3:32:48 PM

© Riverside County RCIT GIS

Notes

My Map



Legend

- Display Parcels
- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 10/19/2016 3:33:29 PM

© Riverside County RCIT GIS

Notes

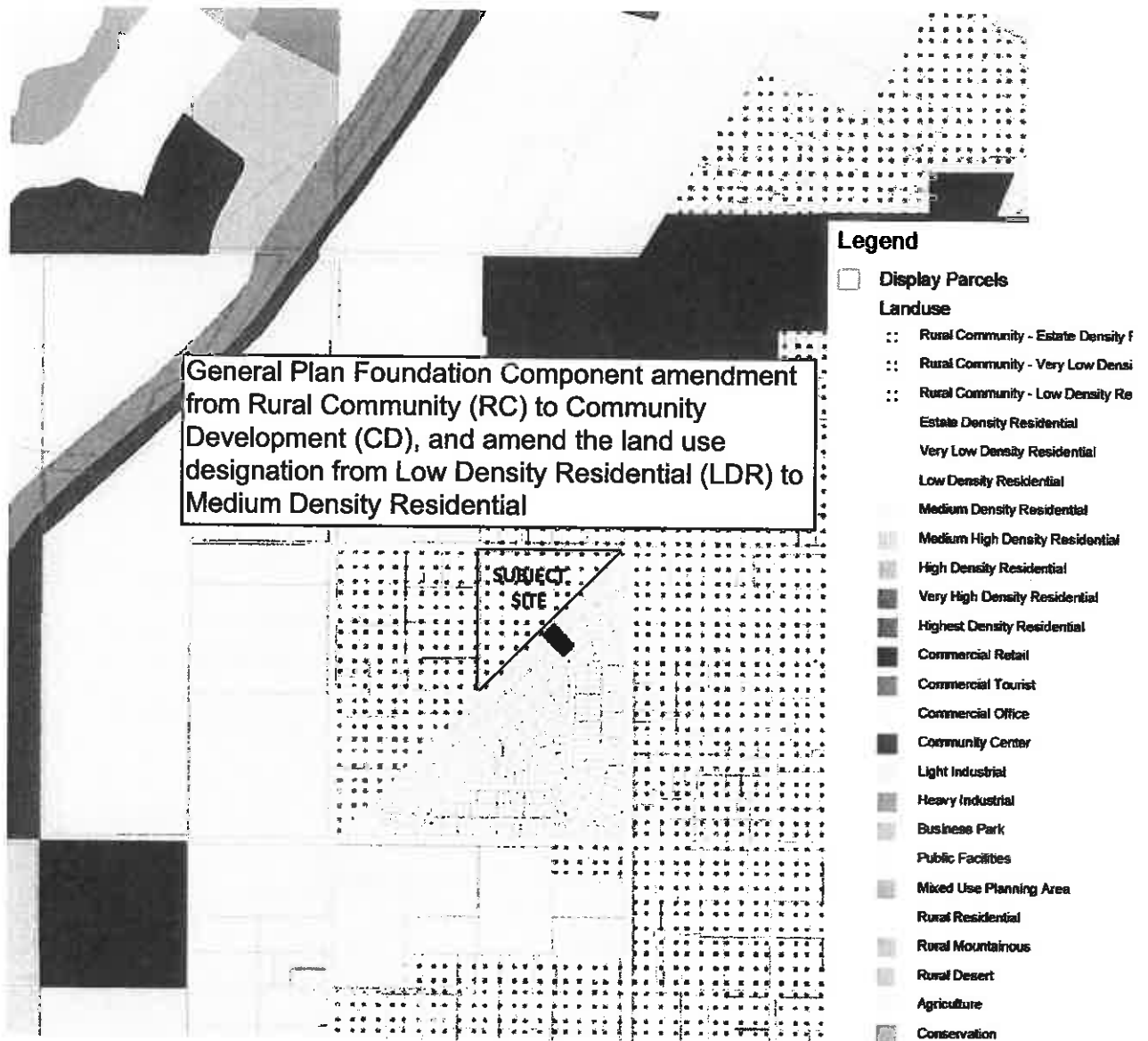
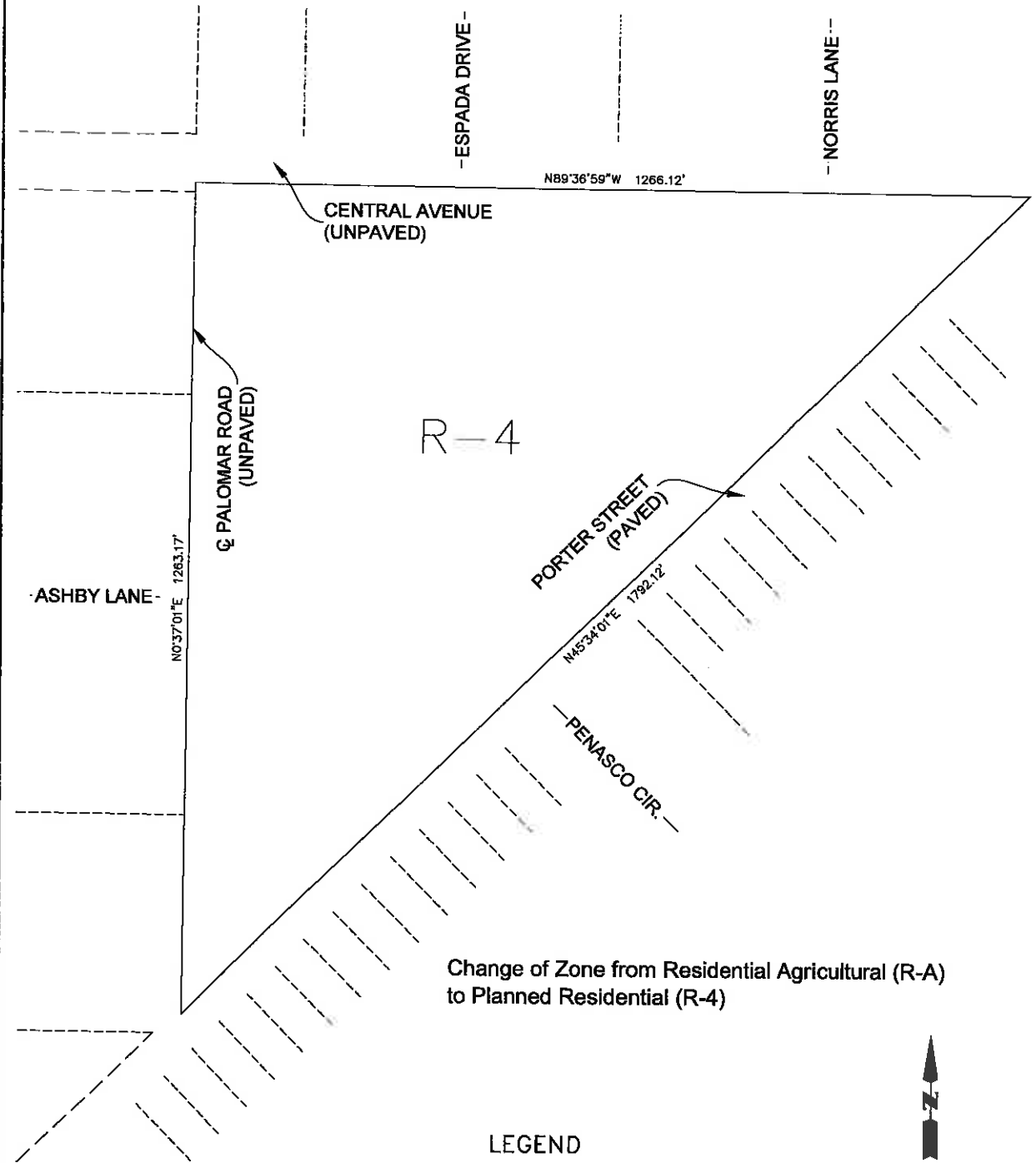


Figure 3 - General Plan Area

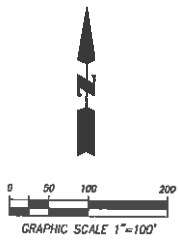
NUEVO AREA
 SEC. 26, T. 4 S., R. 3W. S.B.B. & M



Change of Zone from Residential Agricultural (R-A)
 to Planned Residential (R-4)

LEGEND

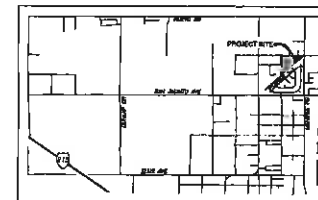
R-4 PLANNED RESIDENTIAL



MAP NO. XX.XX
**CHANGE OF OFFICIAL ZONING PLAN
 AMENDING**
MAP NO. 2 ORDINANCE NO. 348
 CHANGE OF ZONE CASE NO. 7919
 ADOPTED BY ORDINANCE NO. 348
 DATE: _____
 RIVERSIDE COUNTY BOARD OF SUPERVISORS

ASSESSOR'S PARCEL NO'S.: 309-060-001 & 309-060-04

IN THE UNINCORPORATED AREA OF RIVERSIDE COUNTY, STATE OF CALIFORNIA
TENTATIVE MAP - TRACT NO. 37134
 BEING A SUBMISSION OF A PORTION OF SECTION 26, TOWNSHIP 4 SOUTH
 RANGE 3 WEST, SAN BERNARDINO BASE AND MERIDIAN
 UNITED ENGINEERING GROUP CA., INC AUGUST 2018



VICINITY MAP
 T4S, R3W, SEC. 26

LOT AREAS
 IN SQUARE FEET AND ACRES

LOT #	Area-Sq Feet	Area-Acres	LOT #	Area-Sq Feet	Area-Acres
1	8,837	0.20	11	8,837	0.20
2	8,837	0.20	12	8,837	0.20
3	8,837	0.20	13	8,837	0.20
4	8,837	0.20	14	8,837	0.20
5	8,837	0.20	15	8,837	0.20
6	8,837	0.20	16	8,837	0.20
7	8,837	0.20	17	8,837	0.20
8	8,837	0.20	18	8,837	0.20
9	8,837	0.20	19	8,837	0.20
10	8,837	0.20	20	8,837	0.20
21	8,837	0.20	31	8,837	0.20
22	8,837	0.20	41	8,837	0.20
23	8,837	0.20	51	8,837	0.20
24	8,837	0.20	61	8,837	0.20
25	8,837	0.20	71	8,837	0.20
26	8,837	0.20	81	8,837	0.20
27	8,837	0.20	91	8,837	0.20
28	8,837	0.20	101	8,837	0.20
29	8,837	0.20	111	8,837	0.20
30	8,837	0.20	121	8,837	0.20
31	8,837	0.20	131	8,837	0.20
32	8,837	0.20	141	8,837	0.20
33	8,837	0.20	151	8,837	0.20
34	8,837	0.20	161	8,837	0.20
35	8,837	0.20	171	8,837	0.20
36	8,837	0.20	181	8,837	0.20
37	8,837	0.20	191	8,837	0.20
38	8,837	0.20	201	8,837	0.20
39	8,837	0.20	211	8,837	0.20
40	8,837	0.20	221	8,837	0.20
41	8,837	0.20	231	8,837	0.20
42	8,837	0.20	241	8,837	0.20
43	8,837	0.20	251	8,837	0.20
44	8,837	0.20	261	8,837	0.20
45	8,837	0.20	271	8,837	0.20
46	8,837	0.20	281	8,837	0.20
47	8,837	0.20	291	8,837	0.20
48	8,837	0.20	301	8,837	0.20
49	8,837	0.20	311	8,837	0.20
50	8,837	0.20	321	8,837	0.20
51	8,837	0.20	331	8,837	0.20
52	8,837	0.20	341	8,837	0.20
53	8,837	0.20	351	8,837	0.20
54	8,837	0.20	361	8,837	0.20
55	8,837	0.20	371	8,837	0.20
56	8,837	0.20	381	8,837	0.20
57	8,837	0.20	391	8,837	0.20
58	8,837	0.20	401	8,837	0.20
59	8,837	0.20	411	8,837	0.20
60	8,837	0.20	421	8,837	0.20
61	8,837	0.20	431	8,837	0.20
62	8,837	0.20	441	8,837	0.20
63	8,837	0.20	451	8,837	0.20
64	8,837	0.20	461	8,837	0.20
65	8,837	0.20	471	8,837	0.20
66	8,837	0.20	481	8,837	0.20
67	8,837	0.20	491	8,837	0.20
68	8,837	0.20	501	8,837	0.20
69	8,837	0.20	511	8,837	0.20
70	8,837	0.20	521	8,837	0.20
71	8,837	0.20	531	8,837	0.20
72	8,837	0.20	541	8,837	0.20
73	8,837	0.20	551	8,837	0.20
74	8,837	0.20	561	8,837	0.20
75	8,837	0.20	571	8,837	0.20
76	8,837	0.20	581	8,837	0.20
77	8,837	0.20	591	8,837	0.20
78	8,837	0.20	601	8,837	0.20
79	8,837	0.20	611	8,837	0.20
80	8,837	0.20	621	8,837	0.20
81	8,837	0.20	631	8,837	0.20
82	8,837	0.20	641	8,837	0.20
83	8,837	0.20	651	8,837	0.20
84	8,837	0.20	661	8,837	0.20
85	8,837	0.20	671	8,837	0.20
86	8,837	0.20	681	8,837	0.20
87	8,837	0.20	691	8,837	0.20
88	8,837	0.20	701	8,837	0.20
89	8,837	0.20	711	8,837	0.20
90	8,837	0.20	721	8,837	0.20
91	8,837	0.20	731	8,837	0.20
92	8,837	0.20	741	8,837	0.20
93	8,837	0.20	751	8,837	0.20
94	8,837	0.20	761	8,837	0.20
95	8,837	0.20	771	8,837	0.20
96	8,837	0.20	781	8,837	0.20
97	8,837	0.20	791	8,837	0.20
98	8,837	0.20	801	8,837	0.20
99	8,837	0.20	811	8,837	0.20
100	8,837	0.20	821	8,837	0.20
101	8,837	0.20	831	8,837	0.20
102	8,837	0.20	841	8,837	0.20
103	8,837	0.20	851	8,837	0.20
104	8,837	0.20	861	8,837	0.20
105	8,837	0.20	871	8,837	0.20
106	8,837	0.20	881	8,837	0.20
107	8,837	0.20	891	8,837	0.20
108	8,837	0.20	901	8,837	0.20
109	8,837	0.20	911	8,837	0.20
110	8,837	0.20	921	8,837	0.20
111	8,837	0.20	931	8,837	0.20
112	8,837	0.20	941	8,837	0.20
113	8,837	0.20	951	8,837	0.20
114	8,837	0.20	961	8,837	0.20
115	8,837	0.20	971	8,837	0.20
116	8,837	0.20	981	8,837	0.20
117	8,837	0.20	991	8,837	0.20
118	8,837	0.20	1001	8,837	0.20
119	8,837	0.20	1011	8,837	0.20
120	8,837	0.20	1021	8,837	0.20
121	8,837	0.20	1031	8,837	0.20
122	8,837	0.20	1041	8,837	0.20
123	8,837	0.20	1051	8,837	0.20
124	8,837	0.20	1061	8,837	0.20
125	8,837	0.20	1071	8,837	0.20
126	8,837	0.20	1081	8,837	0.20
127	8,837	0.20	1091	8,837	0.20
128	8,837	0.20	1101	8,837	0.20
129	8,837	0.20	1111	8,837	0.20
130	8,837	0.20	1121	8,837	0.20
131	8,837	0.20	1131	8,837	0.20
132	8,837	0.20	1141	8,837	0.20
133	8,837	0.20	1151	8,837	0.20
134	8,837	0.20	1161	8,837	0.20
135	8,837	0.20	1171	8,837	0.20
136	8,837	0.20	1181	8,837	0.20
137	8,837	0.20	1191	8,837	0.20
138	8,837	0.20	1201	8,837	0.20
139	8,837	0.20	1211	8,837	0.20
140	8,837	0.20	1221	8,837	0.20
141	8,837	0.20	1231	8,837	0.20
142	8,837	0.20	1241	8,837	0.20
143	8,837	0.20	1251	8,837	0.20
144	8,837	0.20	1261	8,837	0.20
145	8,837	0.20	1271	8,837	0.20
146	8,837	0.20	1281	8,837	0.20
147	8,837	0.20	1291	8,837	0.20
148	8,837	0.20	1301	8,837	0.20
149	8,837	0.20	1311	8,837	0.20
150	8,837	0.20	1321	8,837	0.20
151	8,837	0.20	1331	8,837	0.20
152	8,837	0.20	1341	8,837	0.20
153	8,837	0.20	1351	8,837	0.20
154	8,837	0.20	1361	8,837	0.20
155	8,837	0.20	1371	8,837	0.20
156	8,837	0.20	1381	8,837	0.20
157	8,837	0.20	1391	8,837	0.20
158	8,837	0.20	1401	8,837	0.20
159	8,837	0.20	1411	8,837	0.20
160	8,837	0.20	1421	8,837	0.20
161	8,837	0.20	1431	8,837	0.20
162	8,837	0.20	1441	8,837	0.20
163	8,837	0.20	1451	8,837	0.20
164	8,837	0.20	1461	8,837	0.20
165	8,837	0.20	1471	8,837	0.20
166	8,837	0.20	1481	8,837	0.20
167	8,837	0.20	1491	8,837	0.20
168	8,837	0.20	1501	8,837	0.20
169	8,837	0.20	1511	8,837	0.20
170	8,837	0.20	1521	8,837	0.20
171	8,837	0.20	1531	8,837	0.20
172	8,837	0.20	1541	8,837	0.20
173	8,837	0.20	1551	8,837	0.20
174	8,837	0.20	1561	8,837	0.20
175	8,837	0.20	1571	8,837	0.20
176	8,837	0.20	1581	8,837	0.20
177	8,837	0.20	1591	8,837	0.20
178	8,837	0.20	1601	8,837	0.20
179	8,837	0.20	1611	8,837	0.20
180	8,837	0.20	1621	8,837	0.20
181	8,837	0.20	1631	8,837	0.20
182	8,837	0.20	1641	8,837	0.20
183	8,837	0.20	1651	8,837	0.20
184	8,837	0.20	1661	8,837	0.20
185	8,837	0.20	1671	8,837	0.20
186	8,837	0.20	1681	8,837	0.20
187	8,837	0.20	1691	8,837	0.20
188	8,837	0.20	1701	8,837	0.20
189	8,837	0.20	1711	8,837	0.20
190	8,837	0.20	1721	8,837	0.20
191	8,837	0.20	1731	8,837	0.20
192	8,837	0.20	1741	8,837	0.20
193	8,837	0.20	1751	8,837	0.20
194	8,837	0.20	1761	8,837	0.20
195	8,837	0.20	1771	8,837	0.20
196	8,837	0.20	1781	8,837	0.20
197	8,837	0.20	1791	8,837	0.20
198	8,837	0.20	1801	8,837	0.20
199	8,837	0.20	1811	8,837	0.20
200	8,837	0.20	1821	8,837	0.20
201	8,837	0.20	1831	8,837	0.20
202	8,837	0.20	1841	8,837	0.20
203	8,837	0.20	1851	8,837	0.20
204	8,837	0.20	1861	8,837	0.20
205	8,837	0.20	1871	8,837	0.20
206	8,837	0.20	1881	8,837	0.20
207	8,837	0.20	1891	8,837	0.20
208	8,837	0.20	1901	8,837	0.20
209	8,837	0.20	1911	8,837	0.20
210	8,837	0.20	1921	8,837	0.20
211	8,837	0.20	1931	8,837	0.20
212	8,837	0.20	1941	8,837	0.20
213	8,837	0.20	1951	8,837	0.20
214					

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

A regular scheduled meeting of the Airport Land Use Commission was held on November 10, 2016 at the Riverside County Administrative Center, Board Chambers.

COMMISSIONERS PRESENT: Simon Housman, Chairman
Rod Ballance, Vice Chairman
Russell Betts
Arthur Butler
Glen Holmes
John Lyon
Linda Krupa, Alternate for Steve Manos

COMMISSIONERS ABSENT: Steve Manos

STAFF PRESENT: Ed Cooper, ALUC Director
John Guerin, Principal Planner
Paul Rull, Urban Regional Planner IV
Barbara Santos, ALUC Commission Secretary
Raymond Mistica, ALUC Counsel

OTHERS PRESENT: Deanna Elliano, City of Hemet
Monica Estevey, Other Interested Person
Nick Johnson, Other Interested Person
Mark Quental, Watermark Development
Rick Sandzimier, City of Moreno Valley

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

- I. **AGENDA ITEM 2.1: ZAP1206MA16 – Optimus Building Corporation (Representative: Gary Hamro)** – City of Perris Case No. DPR 14-01-0015 (Development Plan Review). The applicant is proposing a revision to plans for a two-building warehousing project previously determined to be consistent pursuant to ALUC Case No. ZAP1102MA14, specifically to increase the office area in Building A (which is not located in an Accident Potential Zone) from 15,000 square feet to 45,000 square feet. As amended, Building A would provide 45,000 square feet of office area and 867,338 square feet of warehouse area. No changes are proposed for Building B. The overall floor areas of Buildings A (912,338 square feet) and B (125,437 square feet) would remain the same as originally proposed. The 53.56-acre (gross) project site is located easterly of Patterson Avenue, northerly of Markham Street, westerly of Webster Avenue, and southerly of Nance Street (Airport Compatibility Zones B1-APZ I, B2, and C1 of the March Air Reserve Base/Inland Port Airport Influence Area). Continued from September 8, August 11 and October 13, 2016.

II. **MAJOR ISSUES**

The United States Air Force submitted a comment letter dated August 8, 2016, outlining their concerns with the project, specifically regarding Building B in the Accident Potential Zone I (APZ I). This project was previously reviewed by ALUC and found consistent, and no changes to Building B are proposed in this ALUC application, but the project has not yet received City approval. The ALUC staff report for the original project, ZAP1102MA14, determined that 86 or 93 people would be present in the most intense single-acre portion of Building B in Zone APZ I. The Airport Land Use Commission reduced the office area in Building B from 10,000 square feet to 2,500 square feet in Zone APZ I, reducing the single-acre intensity to approximately 74 people. This is consistent with the March Air Reserve Base/Inland Port Airport Compatibility Plan adopted in November 2014. However, since the adoption of the plan, there has been an evolution in the Air Force's position regarding the intent of the APZ I standard of 25 people per acre, as specified in Department of Defense Instruction (DoDI) 4165.57 dated March 12, 2015 and Air Force Instruction (AFI) 32-7063 dated December 18, 2015. These issues may ultimately need to be addressed through an amendment to the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan.

In July, ALUC issued a determination of consistency for ZAP1199MA16, a proposal by the City of Perris to amend the City's General Plan, the Perris Valley Commerce Center Specific Plan, and the City's Zoning Ordinance to bring those documents into consistency with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan. That proposal has been tentatively approved by the Perris City Council. Final adoption is expected to occur prior to ALUC's November 10 meeting. Upon final adoption of the Citywide and area-wide amendments noted above, projects such as DPR 14-01-0015, which do not require site-specific general plan amendments, specific plan amendments, or zone changes, will be able to be evaluated for ALUCP consistency by the City of Perris without a mandatory ALUC referral. It is the applicant's intent to utilize the new process that will become available to projects in that City once final adoption of the Citywide and area-wide amendments has occurred. The applicant has requested continuance to ALUC's DECEMBER 8 meeting with an eye toward possible withdrawal of this ALUC application prior to DECEMBER 8.

III. **STAFF RECOMMENDATION**

At this time per the applicant's request, staff recommends that the Commission CONTINUE consideration of this matter to its DECEMBER 8, 2016 public hearing agenda.

IN THE ABSENCE OF THE REQUEST FOR CONTINUANCE, STAFF WOULD HAVE RECOMMENDED THAT THE REVISIONS TO THE DEVELOPMENT PLAN REVIEW BE FOUND CONSISTENT, SUBJECT TO THE REVISED CONDITIONS INCLUDED HEREIN.

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

IV. PROJECT DESCRIPTION

The applicant is proposing a revision to plans for a two-building warehousing project previously determined to be consistent pursuant to ALUC Case No. ZAP1102MA14, specifically to increase the office area in Building A (which is not located in an Accident Potential Zone) from 15,000 square feet to 45,000 square feet. As amended, Building A would provide 45,000 square feet of office area and 867,338 square feet of warehouse area. No changes are proposed for Building B. The overall floor areas of Buildings A (912,338 square feet) and B (125,437 square feet) would remain the same as originally proposed.

V. MEETING SUMMARY

The following staff presented the subject proposal:

ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at pruil@rctlma.org

No one spoke in favor, neutral or opposition to the project

VI. ALUC COMMISSION ACTION

The ALUC Commission by a unanimous vote of 7-0 **CONTINUED** the project to December 8, 2016.

VII. CD

The entire discussion of this agenda item can be found on CD and referenced by the meeting time listed below. For a copy of the CD, please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at basantos@rctlma.org.

ITEM 2.1: TIME 9:03 A.M.

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

I. **AGENDA ITEM 2.2: ZAP1204MA16 – Duke Realty, Adam Schmid (Representative: Albert A. Webb Associates, Nicole Torstvet)** – City of Perris Case No. PLN 16-00008 (Development Plan Review). The applicant is proposing to develop a 668,381 square foot warehouse/distribution center on 30.7 acres. The building floor plan consists of 649,481 square feet of warehouse area and 19,200 square feet of office area. The project site is located southerly of Markham Street, westerly of Indian Avenue, easterly of Barrett Avenue and northerly of Perry Street. (Airport Compatibility Zones B1-APZ I, B2, and C1 of the March Air Reserve Base/Inland Port Airport Influence Area). Continued from September 8, August 11, and October 13, 2016.

II. **MAJOR ISSUES**

The applicant is proposing to construct a warehouse/distribution center on a site located predominately within the portion of Airport Compatibility Zone B1 that lies within Accident Potential Zone I (APZ I), as delineated by the United States Air Force in the 2005 Air Installation Compatible Use Zone (AICUZ) study. The project results in a single-acre intensity of 56 or 66 people in Zone B1-APZ I, which is consistent with the March Air Reserve Base/Inland Port Airport Compatibility Plan adopted in November 2014. However, since the adoption of the plan, there has been an evolution in the Air Force's position regarding the intent of the APZ I standard of 25 people per acre, as specified in Department of Defense Instruction (DoDI) No. 4165.57 dated March 12, 2015 and Air Force Instruction (AFI) 32-7063 dated December 18, 2015. These new documents may ultimately need to be addressed through an amendment to the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan.

In July, ALUC issued a determination of consistency for ZAP1199MA16, a proposal by the City of Perris to amend the City's General Plan, the Perris Valley Commerce Center Specific Plan, and the City's Zoning Ordinance to bring those documents into consistency with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan. That proposal has been tentatively approved by the Perris City Council. Final adoption is expected to occur prior to ALUC's November 10 meeting. Upon final adoption of the Citywide and area-wide amendments noted above, projects such as PLN 16-00008, which do not require site-specific general plan amendments, specific plan amendments, or zone changes, will be able to be evaluated for ALUCP consistency by the City of Perris without a mandatory ALUC referral. It is the applicant's intent to utilize the new process that will become available to projects in that City once final adoption of the Citywide and area-wide amendments has occurred. The applicant has requested continuance to ALUC's DECEMBER 8 meeting with an eye toward possible withdrawal of this ALUC application prior to DECEMBER 8.

III. **STAFF RECOMMENDATION**

The applicant is requesting additional time to address issues raised by the Air Force with City staff, therefore, staff recommends that the Commission CONTINUE consideration of the matter to its DECEMBER 8, 2016 public hearing agenda.

IV. **PROJECT DESCRIPTION**

The Development Plan Review is a proposal to develop a 668,681 square foot warehouse/distribution center on 30.7 net acres. The building floor plan consists of 649,481 square feet of warehouse area and 19,200 square feet of office area. (The applicant is also proposing a tentative parcel map [PLN 16-05150] to merge the six existing parcels comprising the site into one lot. The parcel map does not require ALUC review – no new lots created.)

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

V. MEETING SUMMARY

The following staff presented the subject proposal:

ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rctlma.org

No one spoke in favor, neutral or opposition to the project

VI. ALUC COMMISSION ACTION

The ALUC Commission by a unanimous vote of 7-0 **CONTINUED** the project to December 8, 2016.

VII. CD

The entire discussion of this agenda item can be found on CD and referenced by the meeting time listed below. For a copy of the CD, please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at basantos@rctlma.org.

ITEM 2.2: TIME: 9:03 A.M.

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

- I. **AGENDA ITEM 2.3:** ZAP1064BD16 – Shadow Hills RV Resort (Representative: Coachella Valley Engineers) – City of Indio Planning Case No. CUP 16-1-1012 (Conditional Use Permit). The applicant is proposing to modify and expand the operation of the existing Shadow Hills RV Resort and establish guidelines for special events that temporarily allow additional RV camping and tent campsites on 14 acres. The project site is located westerly of Jefferson Street and northerly of Varner Road. (Airport Compatibility Zone D of the Bermuda Dunes Airport Influence Area). Continued from October 13, 2016.

II. **MAJOR ISSUES**

On October 13, 2016, the Commission continued the project to its November 10, 2016, meeting to allow time for the applicant to meet with staff to discuss their concerns with project conditions. Staff met with the applicant and revised the conditions as follows:

- Conditions were separated into two categories: conditions that apply to the 4 acre expansion area; and conditions that apply to the entire 14 acre project site. Conditions Nos. 1, 2, 4, and 6 shall only apply to the 4 acre expansion area. Conditions Nos. 5, 7, 8, and 9 shall apply to the entire 14 acre project site.
- Condition No. 3 was removed.
- Minor edits to Conditions Nos. 1, 2e, 5, 8, and 9 to provide additional clarification.
- Condition No. 6 was reworded to state that the applicant would submit to the FAA review if new RV heights and site elevations together exceed 70 feet above mean sea level.

The existing RV resort was established prior to the adoption of the Bermuda Dunes ALUCP and qualifies as a pre-existing use. The applicant is proposing to utilize 4 acres of an adjacent property for temporary usage during events. Should the expansion be approved, 4 acres of land would remain vacant for most of the year. However, during special events (which may ultimately be up to 50 days per year, generally not more than five consecutive days), this 4-acre area would be used for temporary RV spaces or tents. During these special events, a combination of up to 700 camping tents and 259 RV spaces could result in exceedance of Compatibility Zone D single-acre intensity limits if not dispersed. In order for the project to be consistent, a condition has been applied to ensure that the tents are dispersed so as not to exceed the single acre criterion.

In addition, staff received a comment letter dated September 28, 2016, from the property owner of the adjacent 4 acre expansion property, Joseph Tennant of Jefferson Desert St. Property, LLC. This entity is proposing to develop a business park on its acreage (including the 4-acre area in question) and is requesting that the 4-acre area be deleted from ALUC's action on this project. He also provided a copy of a notice issued to Shadow Hills RV Resort on June 2, 2016, requesting vacation of the leased property within 90 days. The applicant contends that this notice is not consistent with the terms of his lease.

ALUC's role is solely to determine whether the proposal at hand is or is not consistent with the adopted Compatibility Plan, and to advise the local jurisdiction accordingly.

III. **STAFF RECOMMENDATION**

Staff recommends that the Conditional Use Permit be found CONSISTENT, subject to the conditions included herein.

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

IV. PROJECT DESCRIPTION

The applicant proposes to modify and expand the operation of the existing Shadow Hills RV Resort and establish guidelines for special events that would allow additional RV camping on an adjacent 4-acre area and provision for up to 700 tents within the 14-acre area. The applicant is proposing to increase the number of permanent RV spaces from 124 to 135 on the existing 10 acre parcel, provide for an additional 124 spaces on a temporary basis (during special events) located on the adjacent 4 acre parcel to the west, resulting in a total 259 spaces, and allow up to 700 camping tents onsite during major special events to occur up to 50 days per year. The project is divided into two portions: the existing RV park located on a 10 acre parcel, and an adjacent 4 acre parcel which will be used as temporary RV spaces during major special events. The existing RV park currently contains 124 permanent RV spaces. The applicant proposes to add 11 permanent RV spaces. The applicant is also proposing that 700 camping tents be allowed onsite during major special events. (Tent spaces in the 4-acre area will be 300 square feet in area [10 feet by 30 feet] and will include vehicle parking area for the tent campers. Some tent spaces in the existing 10-acre RV Resort may be as small as 200 square feet.) The applicant proposes to have 124 temporary RV spaces on the adjacent 4 acre parcel during special events. In total, there will be some combination of up to 259 RV spaces and up to 700 camping tents during peak time occupancy, but the maximum levels of each would not occur simultaneously. Excluding special events, the Resort is limited to a maximum occupancy of 1,250 persons, as conditioned by the City of Indio.

CONDITIONS:

The following conditions shall only apply to the 4 acre expansion area.

1. Any permanent or large scale outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Lighting associated with personal use (camp lanterns, RV lights) is not included.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use or activity which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use or activity which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use or activity which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, composting operations, production of cereal grains, sunflower, and row crops, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

- (e) Outdoor nonresidential uses that are susceptible to high noise impacts.
- 3. Any new detention basin(s) on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
- 4. **The applicant shall submit to FAA for review at locations within the 4-acre site where site elevations plus new RV height together would exceed 70 feet above mean sea level (AMSL).**

The following conditions shall apply to the entire 14 acre project site.

- 5. The total on-site occupancy shall not exceed 1,400 people, unless the applicable Compatibility Plan is modified so as to increase permissible average intensity levels in Compatibility Zone D to a higher level. If this number were to be exceeded **for more than 12 days in any given calendar year**, the project shall be brought back to ALUC for review.
- 7. The maximum allowable intensity within any single acre of the site (excluding RV spaces in existence as of 2004) shall be limited to 300 persons.
- 8. The tent camping area shall be dispersed over the 14-acre project site, such that not more than 145 tents are placed in any one **square** acre area.
- 9. Open area requirements were not applied to this project as the 4 acre expansion is below the required threshold of 10 acres or greater. However, any additional or further project expansions in the future **other than open parking areas that meet ALUC open area requirements** will be considered by ALUC in context with the existing 10 acre RV park and this project's 4 acre expansion area, which will trigger open area requirements.

V. MEETING SUMMARY

The following staff presented the subject proposal:

ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rctlma.org

No one spoke in favor, neutral or opposition to the project

VI. ALUC COMMISSION ACTION

The ALUC Commission by a unanimous vote of 7-0 found the project **CONSISTENT**.

VII. CD

The entire discussion of this agenda item can be found on CD and referenced by the meeting time listed below. For a copy of the CD, please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at basantos@rctlma.org.

ITEM 2.3: TIME: 9:04 A.M.

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

I. **AGENDA ITEM 3.1:** ZAP1081RI16 – Wolf Nejedly V, LLC – City of Riverside Planning Case Nos. P16-0617 (Rezone) and P16-0618 (Design Review). The applicant proposes rezoning of 0.94 acres located northerly of Indiana Avenue, southerly of the 91 freeway, easterly of Jefferson Street and westerly of Winstrom Street from R-1-7000-SP Single Family Residential Specific Plan Overlay to CG-SP Commercial General Specific Plan Overlay. The Commission may further recommend that the site be rezoned to CG-SP-AP-E (Commercial General Specific Plan Overlay, Airport Protection Overlay Zone E). The applicant also proposes to construct a 1,652 square foot addition to an existing 900 square foot building to be used as a sales office for an Enterprise Car Sales dealership. (Airport Compatibility Zone E of the Riverside Municipal Airport Influence Area).

II. **MAJOR ISSUES**

None

III. **STAFF RECOMMENDATION**

Staff recommends that the Rezoning be found CONSISTENT with the 2005 Riverside Municipal Airport Land Use Compatibility Plan, provided that the new zoning incorporates the appropriate Airport Protection Overlay Zone suffix (-AP-E), which refers to the site's location within Airport Compatibility Zone E of the Riverside Municipal Airport Influence Area, and that the Design Review be found CONSISTENT, subject to the conditions included herein.

IV. **PROJECT DESCRIPTION**

The applicant proposes rezoning of 0.94 acres from R1-7000-SP Single Family Residential Specific Plan Overlay to CG-SP Commercial General Specific Plan Overlay and to construct a 1,652 square foot addition to an existing 900 square foot building to be used as a sales office for an Enterprise Car Sales dealership.

CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky.
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

- (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
- 3. The attached notice shall be given to all prospective purchasers and/or tenants of the property.
- 4. No new detention basins are depicted on the site plan. Any new detention basins on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
- 5. Prior to issuance of a building permit, the City of Riverside shall apply zoning incorporating the Airport Protection Overlay Zone (CG-SP-AP-E) to the site.

V. MEETING SUMMARY

The following staff presented the subject proposal:

ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rctlma.org

No one spoke in favor, neutral or opposition to the project

VI. ALUC COMMISSION ACTION

The ALUC Commission by a unanimous vote of 7-0 found the project **CONSISTENT**.

VII. CD

The entire discussion of this agenda item can be found on CD and referenced by the meeting time listed below. For a copy of the CD, please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at basantos@rctlma.org.

ITEM 3.1: TIME: 9:10 A.M.

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

I. **AGENDA ITEM 3.2: ZAP1072FV16 – KTM North America (Representative: MDMG Inc.)** – County of Riverside Planning Case No. PP25714 (Amended Plot Plan). The applicant is proposing to develop a private KTM dirt bike testing facility consisting of a 2,160 square foot building for motorcycle maintenance (with an observation deck) and 5.9 acres of dirt bike test tracks on a 9.07 gross acre parcel. The test track facility will not be open for public use or viewing. The project site is located easterly of Sky Canyon Drive, northerly of Technology Drive, and southerly of Borel Road (Airport Compatibility Zone B1 and C of the French Valley Airport Influence Area).

II. **MAJOR ISSUES**
None

III. **STAFF RECOMMENDATION**
Staff recommends a finding of CONSISTENCY for the Plot Plan, subject to the conditions included herein.

IV. **PROJECT DESCRIPTION**
Amended Plot Plan No. 25714 (PP25714) is a proposal to develop a private KTM dirt bike testing facility consisting of a 2,160 square foot building for motorcycle maintenance (with an observation deck) and 5.9 acres of dirt bike test tracks on a 9.07 gross acre parcel. The height of the building is 25 feet. The test track facility will not be open for public use or viewing.

The Commission had previously found the original proposal for a contractor's storage yard on this parcel consistent via ZAP1059FV14.

CONDITIONS:

1. Any outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky and shall comply with Riverside County Ordinance No. 655 as applicable. Outdoor lighting plans, if any, shall be transmitted to Riverside County Economic Development Agency – Aviation Division personnel and to the French Valley Airport for review and comment. (Failure to comment within thirty days shall be considered to constitute acceptability on the part of the airport manager.) Uses of the dirt bike test track after dusk hours is prohibited.
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) *Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.*
 - (c) *Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area, including landscaping utilizing water features, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, and incinerators.*

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

- (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, day care centers, libraries, hospitals, nursing homes, highly noise-sensitive outdoor non-residential uses, aboveground bulk storage of hazardous materials, critical community infrastructure facilities, and hazards to flight.
3. The attached notice shall be provided to all potential purchasers of the property and/or tenants of the proposed building.
 4. Any new detention basins on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
 5. The project was analyzed as a private dirt bike test track facility which is not open to the public. No use of the dirt bike test track for the purpose of spectator sports, in which guests pay for admission to an event or series of events, or to which the general public is invited, or large gathering of crowds (no more than 160 people per single acre area), is included in this determination of consistency.
 6. Prior to issuance of building permits, the landowner shall convey an avigation easement to the County of Riverside as owner-operator of French Valley Airport, which shall be recorded, or shall provide evidence that such an easement covering the property has already been recorded. Copies of the avigation easement, upon recordation, shall be forwarded to the Riverside County Planning Department and to the Riverside County Airport Land Use Commission.
 7. The Federal Aviation Administration has conducted an aeronautical study of the proposed building/observation deck (Aeronautical Study No. 2015-AWP-8597-OE) and has determined that neither marking nor lighting of the structure will be necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1K Change 2 and shall be maintained in accordance therewith for the life of the project.
 8. The maximum elevation at the top of any proposed structure, including all roof-mounted appurtenances (if any), shall not exceed 1,340 feet above mean sea level. This maximum elevation shall not be increased without further review by the Airport Land Use Commission and the Federal Aviation Administration.
 9. Temporary construction equipment such as cranes used during actual construction of the structure shall not exceed a height of 30 feet unless separate notice is provided to the Federal Aviation Administration through the Form 7460-01 process.
 10. Within five (5) days after construction of the building reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions) This requirement is also applicable in the event the

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

project is abandoned.

- V. **MEETING SUMMARY**
The following staff presented the subject proposal:
ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rctlma.org
- No one spoke in favor, neutral or opposition to the project
- VI. **ALUC COMMISSION ACTION**
The ALUC Commission by a unanimous vote of 7-0 found the project **CONSISTENT**.
- VII. **CD**
The entire discussion of this agenda item can be found on CD and referenced by the meeting time listed below. For a copy of the CD, please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at basantos@rctlma.org.
- ITEM 3.2: TIME: 9:13 A.M.

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

I. **AGENDA ITEM 3.3:** ZAP1215MA16 – Clarion Partners (Representative: Solar City Corporation) – City of Moreno Valley Planning Case No. P16-090 (Plot Plan). The applicant is proposing installation of a 4014.36 kilowatt solar rooftop panel system (ONT6) on the existing 1,173,709 square foot Amazon warehouse/distribution center on a 35.4 acre parcel (Assessor's Parcel Number: 316-170-023) located at 24208 San Michele Road (on the northwest corner of San Michele Road and Indian Avenue). (Airport Compatibility Zone C1 of the March Air Reserve Base/Inland Port Airport Influence Area).

II. **MAJOR ISSUES**
None

III. **STAFF RECOMMENDATION**
Staff recommends that the Plot Plan be found CONSISTENT, subject to the conditions included herein.

IV. **PROJECT DESCRIPTION**
The Plot Plan is a proposal for the installation of a 4014.36 kilowatt solar rooftop panel system (ONT6) on the existing 1,173,709 square foot Amazon warehouse/distribution center on a 35.4 acre parcel.

CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport to the extent as to result in a potential for temporary after-image greater than the low ("green") level.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

- (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, congregate care facilities, hotels/motels, places of assembly (including churches and theaters), buildings with more than 3 aboveground habitable floors, noise sensitive outdoor nonresidential uses, critical community infrastructure facilities and hazards to flight.
- 3. Prior to issuance of any building permits, the landowner shall convey and have recorded an aviation easement to the March Inland Port Airport Authority. Contact March Joint Powers Authority at (951) 656-7000 for additional information.
- 4. The attached notice shall be given to all prospective purchasers of the property and tenants of the building.
- 5. If the panels are mounted on a framework, said framework shall have a flat or matte finish so as to minimize reflection of sunlight.
- 6. All photovoltaic panels installed on the project site shall have received an anti-reflective coating to minimize the potential for hazardous glare to occur to aircraft.
- 7. In the event that any incidence of glint, glare, or flash affecting the safety of air navigation occurs as a result of project operation, upon notification to the airport operator of an incidence, the airport operator shall notify the project operator in writing. Within 30 days of written notice, the project operator shall be required to promptly take all measures necessary to eliminate such glint, glare, or flash. An "incidence" includes any situation that results in an accident, incident, "near-miss," or specific safety complaint regarding an in-flight experience to the airport operator or to federal, state, or county authorities responsible for the safety of air navigation. The project operator shall work with the airport operator to prevent recurrence of the incidence. Suggested measures may include, but are not limited to, reprogramming the alignment of the panels, covering them at the time of day when incidences of glare occur, or wholly removing panels to diminish or eliminate the source of the glint, glare, or flash. For each such incidence made known to the project operator, the necessary remediation shall only be considered to have been fulfilled when the airport operator states in writing that the situation has been remediated to the airport operator's satisfaction.
- 8. In the event that any incidence of electrical interference affecting the safety of air navigation occurs as a result of project operation, upon notification to the airport operator of an incidence, the airport operator shall notify the project operator in writing. Within 30 days of written notice, the project operator shall be required to promptly take all measures necessary to eliminate such interference. An "incidence" includes any situation that results in an accident, incident, "near-miss," report by airport personnel, or specific safety complaint to the airport operator or to federal, state, or county authorities responsible for the safety of air navigation. The project operator shall work with the airport operator to prevent recurrence of the incidence. For each such incidence made known to the project operator, the necessary remediation shall only be considered to have been fulfilled when the airport operator states in writing that the situation has been remediated to the airport operator's satisfaction.
- 9. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

10. The Federal Aviation Administration has conducted aeronautical studies of the proposed structure (Aeronautical Study Nos. 2016-AWP-3704-OE, 2016-AWP-3705-OE, 2016-AWP-3706-OE, 2016-AWP-3707-OE, 2016-AWP-3708-OE, 2016-AWP-3709-OE, 2016-AWP-3710-OE, and 2016-AWP-3711-OE) and has determined that neither marking nor lighting of the structure is necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 L and shall be maintained in accordance therewith for the life of the project.
11. The specific coordinates, height, and top point elevation of the proposed structure shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in structure height or elevation shall not require further review by the Airport Land Use Commission.
12. Temporary construction equipment used during actual construction of the structure shall not exceed the height of the structure, unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.
13. Within five (5) days after construction of the structure reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned or a decision is made not to construct the structure.

V. MEETING SUMMARY

The following staff presented the subject proposal:

ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rcilma.org

No one spoke in favor, neutral or opposition to the project

VI. ALUC COMMISSION ACTION

The ALUC Commission by a unanimous vote of 7-0 found the project **CONSISTENT**.

VII. CD

The entire discussion of this agenda item can be found on CD and referenced by the meeting time listed below. For a copy of the CD, please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at basantos@rcilma.org.

ITEM 3.3: TIME: 9:16 A.M.

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

I. **AGENDA ITEM 3.4:** ZAP1216MA16 – FR Cal Moreno Valley LLC (Representative: Solar City Corporation) – City of Moreno Valley Planning Case No. P16-091 (Plot Plan). The applicant is proposing installation of a 1020.6 kilowatt solar rooftop panel system (ONT8) on the existing 769,320 square foot Amazon warehouse/distribution center on a 35.1 acre parcel (Assessor's Parcel Number: 316-180-013) located at 24300 Nandina Avenue. (on the northwest corner of Nandina Avenue and Indian Avenue). (Airport Compatibility Zone C1 of the March Air Reserve Base/Inland Port Airport Influence Area).

II. **MAJOR ISSUES**
None

III. **STAFF RECOMMENDATION**
RECOMMENDATION: Staff recommends that the Plot Plan be found CONSISTENT, subject to the conditions included herein.

IV. **PROJECT DESCRIPTION**
The Plot Plan is a proposal for the installation of a 1020.6 kilowatt solar rooftop panel system (ONT8) on the existing 769,320 square foot Amazon warehouse/distribution center on a 35.1 acre parcel.

CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport to the extent as to result in a potential for temporary after-image greater than the low ("green") level.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, congregate care facilities, hotels/motels, places of assembly (including churches and

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

theaters), buildings with more than 3 aboveground habitable floors, noise sensitive outdoor nonresidential uses, critical community infrastructure facilities and hazards to flight.

3. Prior to issuance of any building permits, the landowner shall convey and have recorded an aviation easement to the March Inland Port Airport Authority. Contact March Joint Powers Authority at (951) 656-7000 for additional information.
4. The attached notice shall be given to all prospective purchasers of the property and tenants of the building.
5. If the panels are mounted on a framework, said framework shall have a flat or matte finish so as to minimize reflection of sunlight.
6. All photovoltaic panels installed on the project site shall have received an anti-reflective coating to minimize the potential for hazardous glare to occur to aircraft.
7. In the event that any incidence of glint, glare, or flash affecting the safety of air navigation occurs as a result of project operation, upon notification to the airport operator of an incidence, the airport operator shall notify the project operator in writing. Within 30 days of written notice, the project operator shall be required to promptly take all measures necessary to eliminate such glint, glare, or flash. An "incidence" includes any situation that results in an accident, incident, "near-miss," or specific safety complaint regarding an in-flight experience to the airport operator or to federal, state, or county authorities responsible for the safety of air navigation. The project operator shall work with the airport operator to prevent recurrence of the incidence. Suggested measures may include, but are not limited to, reprogramming the alignment of the panels, covering them at the time of day when incidences of glare occur, or wholly removing panels to diminish or eliminate the source of the glint, glare, or flash. For each such incidence made known to the project operator, the necessary remediation shall only be considered to have been fulfilled when the airport operator states in writing that the situation has been remediated to the airport operator's satisfaction.
8. In the event that any incidence of electrical interference affecting the safety of air navigation occurs as a result of project operation, upon notification to the airport operator of an incidence, the airport operator shall notify the project operator in writing. Within 30 days of written notice, the project operator shall be required to promptly take all measures necessary to eliminate such interference. An "incidence" includes any situation that results in an accident, incident, "near-miss," report by airport personnel, or specific safety complaint to the airport operator or to federal, state, or county authorities responsible for the safety of air navigation. The project operator shall work with the airport operator to prevent recurrence of the incidence. For each such incidence made known to the project operator, the necessary remediation shall only be considered to have been fulfilled when the airport operator states in writing that the situation has been remediated to the airport operator's satisfaction.
9. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
10. The Federal Aviation Administration has conducted aeronautical studies of the proposed structure (Aeronautical Study Nos. 2016-AWP-3697-OE, 2016-AWP-3698-OE, 2016-AWP-

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

3699-OE, 2016-AWP-3700-OE, 2016-AWP-3701-OE, and 2016-AWP-3702-OE) and has determined that neither marking nor lighting of the structure is necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 L and shall be maintained in accordance therewith for the life of the project.

11. The specific coordinates, height, and top point elevation of the proposed structure shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in structure height or elevation shall not require further review by the Airport Land Use Commission.
12. Temporary construction equipment used during actual construction of the structure shall not exceed the height of the structure, unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.
13. Within five (5) days after construction of the structure reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned or a decision is made not to construct the structure.

V. MEETING SUMMARY

The following staff presented the subject proposal:

ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rctlma.org

No one spoke in favor, neutral or opposition to the project

VI. ALUC COMMISSION ACTION

The ALUC Commission by a unanimous vote of 7-0 found the project **CONSISTENT**.

VII. CD

The entire discussion of this agenda item can be found on CD and referenced by the meeting time listed below. For a copy of the CD, please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at basantos@rctlma.org.

ITEM 3.4: TIME: 9:29 A.M.

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

I. **AGENDA ITEM 3.5: ZAP1217MA16 – Sares-Regis (Representative: Herdman Rierson Architecture and Design Inc.)** – City of Moreno Valley Planning Case No. P16-0002. The applicant is proposing to amend previously approved ALUC case ZAP1185MA16 to increase the wall heights of the proposed concrete tilt-up warehouse/distribution building from 43 feet to 47.5 feet. The project is located westerly of Indian Street, southerly of its intersection with Grove View Road, easterly of Heacock Street, and northerly of Harley Knox Boulevard (Airport Compatibility Zones B2 and C1 of the March Air Reserve Base/Inland Port Airport Influence Area).

II. **MAJOR ISSUES**
None

III. **STAFF RECOMMENDATION**
Staff recommends that the Site Plan Review be found **CONDITIONALLY CONSISTENT**, subject to the conditions included herein, and such additional conditions as may be required by the Federal Aviation Administration Obstruction Evaluation Service.

IV. **PROJECT DESCRIPTION**
The applicant is proposing to amend previously approved ALUC case ZAP1185MA16 to increase the wall heights of the proposed concrete tilt-up warehouse/distribution building from 43 feet to 47.5 feet.

The Commission found the original proposal to develop a 446,350 square foot manufacturing and warehouse/distribution building on 19.61 acres with a building floor plan consisting of 347,080 square feet of warehouse area, 89,270 square feet of manufacturing area, and 10,000 square feet of second floor mezzanine/office area consistent on May 12, 2016. The original building height was approved at 43 feet and did not require review by the Federal Aviation Administration Obstruction Evaluation Service.

CONDITIONS: Final Conditions await FAA approval

1. Any outdoor lighting installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

- (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities (with overnight stays), congregate care facilities, hotels/motels, places of assembly (including churches and theaters), buildings with more than 3 aboveground habitable floors, noise sensitive outdoor nonresidential uses, critical community infrastructure facilities and hazards to flight.
 - (f) Any reflective materials on the building's rooftop. ***(added by the Commission at meeting dated 11/10/16)***
3. Prior to issuance of any building permits, the landowner shall convey and have recorded an aviation easement to the March Inland Port Airport Authority. Contact March Joint Powers Authority at (951) 656-7000 for additional information.
 4. The attached notice shall be given to all prospective purchasers of the property and tenants of the building.
 5. The proposed detention basins on the site (including water quality management basins) shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the detention basin(s) shall not include trees that produce seeds, fruits, or berries.
 6. This project has been evaluated as a proposal for the establishment of an industrial building (warehouse/manufacturing facility) with not more than 10,000 square feet of office area in any given acre. The City of Moreno Valley shall require additional review by the Airport Land Use Commission prior to the establishment of any of the following uses in this structure:

Auction rooms, auditoriums, bowling alleys, call centers, conference rooms larger than 1,500 square feet in area, classrooms, courtrooms, dance floors, dormitories, drinking establishments, exercise rooms, exhibit rooms, health care facilities (without overnight stays), gymnasiums, locker rooms, lounges, restaurants, retail sales, skating rinks, stages, swimming pools, and all other uses that would be considered to have an occupancy level greater than one person per 100 square feet (minimum square feet per occupant less than 100) pursuant to California Building Code (1998) Table 10-A.
 7. This project has been evaluated as a proposal for 347,080 square feet of warehouse area, 89,270 square feet of manufacturing area, and 10,000 square feet of second floor mezzanine/office area. Any increase in total building area or increase in manufacturing or office area will require review by the Airport Land Use Commission. In addition, this project shall not store, process or manufacture hazardous materials without review and approval by the Airport Land Use Commission.
 8. Noise attenuation measures shall be incorporated into the design of the office areas of the structure, to the extent that such measures are necessary to ensure that interior noise levels from aircraft operations are at or below 45 CNEL.

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

9. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.

V. MEETING SUMMARY

The following staff presented the subject proposal:

ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rctlma.org

The following spoke in favor of the project:

Monica Estevey, Other Interested Person

Nick Johnson, Other Interested Person

The following spoke neither for or against the project, but added information to the decision making process:

Rick Sandzimier, City of Moreno Valley

No one spoke in opposition to the project

VI. ALUC COMMISSION ACTION

The ALUC Commission by a unanimous vote of 7-0 found the project **CONDITIONALLY CONSISTENT** with an additional condition that reflective materials are prohibited on the roof of the structure.

VII. CD

The entire discussion of this agenda item can be found on CD and referenced by the meeting time listed below. For a copy of the CD, please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at basantos@rctlma.org.

ITEM 3.5: TIME: 9:33 A.M.

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

I. **AGENDA ITEM 3.6:** ZAP1018BA14 – Museum of Pinball, Inc. (Representative: Watermark Development Inc.) – City of Banning Planning Case No. CUP-14-8005 (Conditional Use Permit). The applicant is proposing to convert a former manufacturing facility into a pinball museum and arcade for private special events. One of the existing buildings totaling 44,323 square feet would be converted into the museum/arcade and would include exhibit/assembly area, restaurant, bars, seating areas, lounges, and offices. A second and third existing building totaling 73,684 square feet would be maintained for warehouse/storage and office uses. The applicant also proposes to provide for RV camping (43 spaces) and amenities such as a jogging path, swimming pool, and tennis courts. The site consists of approximately 16.45 acres. The project site is located easterly of Hathaway Street, northerly of Westward Avenue, and southerly of Barbour Street (Airport Compatibility Zone D of the Banning Municipal Airport Influence Area).

II. **MAJOR ISSUES**

Since the last Commission meeting in October 2014, ALUC has adopted an amendment to the Banning Municipal Airport Land Use Compatibility Plan, in accordance with a request from the City of Banning to consider increasing allowable nonresidential intensities in Compatibility Zone D. In light of guidelines for nonresidential intensity in the Traffic Pattern Zone included in the 2011 California Airport Land Use Planning Handbook, Compatibility Zone D within the Banning Municipal Airport Influence Area now allows nonresidential intensities up to an average of 200 people per acre and up to 800 people per single acre. Given this expanded allowance, single-acre intensities in Zone D are not eligible to be increased further through use of risk-reduction bonuses. Staff did include a provision allowing for single-acre occupancies up to 1,000 people during special events occurring not more than three days in any calendar year.

The applicant has submitted a revised site plan excluding Assessor's Parcel Number 532-130-003, which was previously proposed to be used as vehicle parking, from the project scope.

The project's single acre intensity using the Building Code method (1,379 people, 958 excluding the areas occupied by the machines) exceeds the recently amended 2016 Banning Airport Land Use Compatibility Plan Zone D criterion of 800 people. However, the arcade and pinball areas will be posted by the City's building official as having a maximum occupancy of 800 persons for the pinball area and 610 persons for the arcade area. The applicant has accepted this and a condition limiting occupancy in any given single acre to 800 people during non-special events and 1000 people during special events permitted 3 days per year.

III. **STAFF RECOMMENDATION**

Staff recommends that the Design Review be found CONSISTENT, subject to the conditions included herein.

IV. **PROJECT DESCRIPTION**

CUP-14-8005 would allow for the conversion of a former manufacturing facility into a pinball museum and arcade *for private special events*. One of the existing buildings totaling 44,323 square feet would be converted into the museum/arcade and would include exhibit/assembly area, restaurant, bars, seating areas, lounges, and offices. A second and third existing building totaling 73,684 square feet would be maintained for warehouse/storage and office uses. The applicant also proposes to provide for RV camping (43 spaces) and amenities such as a jogging path, swimming pool, and tennis courts. The site consists of approximately 16.45 acres.

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

CONDITIONS:

1. Any outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky.
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area, including landscaping utilizing water features, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, and incinerators.
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Highly noise-sensitive outdoor non-residential uses or hazards to flight.
3. The attached notice shall be provided to all potential purchasers of the property, and shall be recorded as a deed notice.
4. Any new retention basins on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the retention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
5. The drive aisle area to be used for ALUC open space area requirements shall be devoid of obstacles/obstructions greater than 4 feet in height or 4 inches in diameter, which includes parking light standards, walls, tall landscaping. **[Amended by ALUC on November 10, 2016]**
6. The project shall only have up to 1,000 people in any given single acre at any given time for special events only three days in any calendar year. The rest of the time during non-special events, the project shall only have up to 800 people in any given single acre at any given time.

V. MEETING SUMMARY

The following staff presented the subject proposal:

ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rcctlma.org

The following spoke in favor of the project:

Mark Quental, Watermark Development, 1838 N. Valencia Ave, Placentia, CA 92870

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

No one spoke in neutral or opposition to the project

VI. ALUC COMMISSION ACTION

The ALUC Commission by a unanimous vote of 6-0 found the project **CONSISTENT** with a change in Condition #5, the obstacles and obstructions shall not be greater than 4 feet in height or 4 inches in diameter. Recuse: Chairman Housman

VII. CD

The entire discussion of this agenda item can be found on CD and referenced by the meeting time listed below. For a copy of the CD, please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at basantos@rctlma.org.

ITEM 3.6: TIME: 9:43 A.M.

**AIRPORT LAND USE COMMISSION
MINUTE ORDER NOVEMBER 10, 2016
RIVERSIDE MEETING**

I. 4.0 ADMINISTRATIVE ITEMS

4.1 Director's Approval – Information Only

II. 5.0 PRESENTATION

Draft Hemet-Ryan Airport Land Use Compatibility Plan – Ken Brody, Mead & Hunt

Ken Brody, Mead & Hunt and John Guerin, ALUC staff gave a brief presentation of the draft Hemet-Ryan Airport Land Use Compatibility Plan. Deanna Elliano, City of Hemet provided comments regarding the draft plan.

III. 6.0 APPROVAL OF MINUTES

The ALUC Commission by a vote of 5-0 approved the October 13, 2016 minutes. Abstained: Commissioners Krupa and Ballance

IV. 7.0 ORAL COMMUNICATION ON ANY MATTER NOT ON THE AGENDA

Chairman Housman commented that Riverside County conducted the Jackie Cochran airshow last weekend that was a phenomenal event. Commissioner Lyon announced that Saturday, November 12th is Flabob's annual Veterans Day Observance, admission is free and everybody is invited to attend. Vice Chairman Ballance announced that today is the two hundred and forty first birthday of the United States Marine Corp.

V. 8.0 COMMISSIONER'S COMMENTS

None

VI. 9.0 ADJOURNMENT

Chairman Housman adjourned the meeting at 10:50 a.m.

VII. CD

The entire discussion of this agenda item can be found on CD and referenced by the meeting time listed below. For a copy of the CD, please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at basantos@rctlma.org.

ITEM 4.0: TIME: 9:54 A.M.