

**AIRPORT LAND USE COMMISSION  
RIVERSIDE COUNTY  
AGENDA**

Riverside County Administrative Center  
4080 Lemon Street, 1st Floor Board Chambers  
Riverside, California

Thursday 9:00 A.M., December 14, 2017



**CHAIR**

Rod Ballance  
Riverside

**VICE CHAIRMAN**

Steve Manos  
Lake Elsinore

**COMMISSIONERS**

Arthur Butler  
Riverside

John Lyon  
Riverside

Glen Holmes  
Hemet

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Desert Hot Springs

Steven Stewart  
Palm Springs

**STAFF**

Director  
Simon A. Housman

John Guerin  
Paul Rull  
Barbara Santos

County Administrative Center  
4080 Lemon St, 14<sup>th</sup> Floor  
Riverside, CA 92501  
(951) 955-5132

[www.rcaluc.org](http://www.rcaluc.org)

**NOTE:** If you wish to speak, please complete a "SPEAKER IDENTIFICATION FORM" and give it to the Secretary. The purpose of the public hearing is to allow interested parties to express their concerns. Comments shall be limited to 5 minutes and to matters relevant to the item under consideration. Please do not repeat information already given. If you have no additional information, but wish to be on record, simply give your name and address and state that you agree with the previous speaker(s). Also please be aware that the indicated staff recommendation shown below may differ from that presented to the Commission during the public hearing.

*Non-exempt materials related to an item on this agenda submitted to the Airport Land Use Commission or its staff after distribution of the agenda packet are available for public inspection in the Airport Land Use Commission's office located at 4080 Lemon Street, 14<sup>th</sup> Floor, Riverside, CA 92501 during normal business hours.*

Live Streaming of the meeting will be available during the meeting on our website at [www.rcaluc.org](http://www.rcaluc.org).

In compliance with the Americans with Disabilities Act, if any accommodations are needed, please contact Barbara Santos at (951) 955-5132 or E-mail at [basantos@rivco.org](mailto:basantos@rivco.org). Request should be made at least 48 hours or as soon as possible prior to the scheduled meeting.

**1.0 INTRODUCTIONS**

**1.1 CALL TO ORDER**

**1.2 SALUTE TO FLAG**

**1.3 ROLL CALL**

**2.0 PUBLIC HEARING: CONTINUED ITEM**

NONE

**3.0 PUBLIC HEARING: NEW ITEMS**

**RIVERSIDE MUNICIPAL AIRPORT**

**3.1 ZAP1091RI17 – Pelican Communities, Richard Hamm** – City of Riverside Planning Case Nos. P17-0467 (General Plan Amendment), P17-0466 (Specific Plan Amendment), P17-0468 (Rezone), P17-0469 (Site Plan Review). P17-0469 is a proposal to construct four three-story buildings with a total floor area of 98,608 square feet providing 108 apartment units (with clubhouse, fitness center, and leasing office) and 1,200 square feet of commercial floor area on 3.14 acres located northerly of Merrill Avenue, westerly of Riverside Avenue and easterly of De Anza Avenue, on the opposite side of Merrill Avenue from Riverside Plaza. The applicant also proposes to: (1) amend the Magnolia Avenue Specific Plan to include Mixed Use Urban (MU-U) as a General Plan land use designation in the Magnolia Center District (P17-0466); (2) amend the City's General Plan land use map designation of the site from Commercial (C) to Mixed

Use Urban (MU-U) (P17-0467), and (3) rezone the site from Commercial General with Specific Plan (Magnolia Avenue) Overlay (CG-SP) to Mixed Use Urban with Specific Plan (Magnolia Avenue) Overlay Zone (MU-U-SP) (P17-0468). (Compatibility Zone E of the Riverside Municipal Airport Influence Area). ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at [prull@rivco.org](mailto:prull@rivco.org)

Staff Recommendation: CONSISTENT

### **CORONA MUNICIPAL AIRPORT**

- 3.2 ZAP1008CO17 – Rowdy Williamson – City of Corona Planning Case Nos. GPA17-001 (General Plan Amendment), CZ17-002 (Change of Zone), CUP17-004 (Conditional Use Permit). The applicant proposes to establish a 64-unit, three-story senior apartment complex on 2.2 acres located at 159 and 205 Buena Vista Avenue, southerly of Railroad Street, northerly of 91 Freeway, and westerly of Vicentia Avenue. The applicant also proposes to amend the General Plan land use designation for this site from Medium Density Residential (MDR) to High Density Residential (HDR) and to change the site's zoning classification from Single Family Residential (R-1-7.2) and Low Density Multi-Family Residential (R-2) to High Density Multi-Family Residential (R-3). (Airport Compatibility Zone D of the Corona Municipal Airport Influence Area). ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at [prull@rivco.org](mailto:prull@rivco.org)

Staff Recommendation: CONSISTENT

### **BERMUDA DUNES AIRPORT**

- 3.3 ZAP1072BD17 – PV Indio, LLC (Representative: Larry Vesely) – City of Indio Planning Case No. DR 17-09-420 (Design Review). The applicant proposes installing canopies over most of the recreational vehicle (RV)/boat storage parking spaces at an existing 4.5 acre self-storage/RV/boat storage facility located at 42925 Madison Street, westerly of Madison Street, southerly of Indio Boulevard and the southerly terminus of Madio Street, and northerly of Fox Glove Lane. The applicant also proposes installing a photovoltaic solar panel system on the roof of two of the proposed canopies. (Airport Compatibility Zones B1 and C of the Bermuda Dunes Airport Influence Area). ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at [prull@rivco.org](mailto:prull@rivco.org)

Staff Recommendation: CONSISTENT

### **JACQUELINE COCHRAN REGIONAL AIRPORT**

- 3.4 ZAP1040TH17 – Coachella Green, LLC (Representative: The Altum Group) – City of Coachella Planning Case Nos. CZ 17-01 (Change of Zone), CUP 279 (Conditional Use Permit), AR 17-03 (Architectural Review). The applicant proposes to change the zoning of 49.19 acres located southerly of Industrial Way, easterly of Enterprise Way, northerly of Avenue 54 and westerly of the Whitewater River Storm Channel from M-H-IP (Heavy Industrial – Industrial Park Overlay) to M-S-IP (Manufacturing Service - Industrial Park Overlay). AR17-03 is a proposal to build a fourteen-building industrial complex on this site with a total building area of 644,567 square feet, anticipated to be built in five phases. CUP 279 is a proposal to allow cannabis cultivation, processing, testing, manufacturing, and distribution uses within the proposed buildings on this property. (Currently Buildings A, J, and K are proposed to be utilized for cannabis cultivation and related uses as described above. Tenants/uses for the remaining buildings have not been identified.) No dispensaries are proposed. (Airport Compatibility Zones C and D of the Jacqueline Cochran Regional Airport Influence Area). ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at [prull@rivco.org](mailto:prull@rivco.org)

Staff Recommendation: CONSISTENT

**JACQUELINE COCHRAN REGIONAL AIRPORT**

- 3.5 ZAP1039TH17 – Thermal Operating Company, LLC, dba The Thermal Club (Representative: Melissa Perez, Albert A. Webb and Associates) - County of Riverside Planning Case Nos. SP00303A4 (Specific Plan Amendment) and CZ07952 (Change of Zone). SP00303A4 is a proposal to amend the Kohl Ranch Specific Plan (most notably the Executive Summary, Project-Wide Planning Standards, and Land Use Planning & Development Standards sections) by defining and establishing a new use category, “racetrack recreational units,” as a permitted land use within Planning Area E-2 located in the portion of the Specific Plan northerly of Avenue 62, easterly of Tyler Street, and westerly of Polk Street within the “Thermal Club.” If approved, this amendment would allow overnight stays at all of the potential development sites on the Thermal Club “founders’ lots.” CZ07952 is a proposal to revise the Specific Plan Zoning Ordinance text in accordance with this proposed Specific Plan Amendment. (Airport Compatibility Zones C and D of the Jacqueline Cochran Regional Airport Influence Area). ALUC Staff Planner: John Guerin at (951) 955-0982, or e-mail at [jguerin@rivco.org](mailto:jguerin@rivco.org)

Staff Recommendation: INCONSISTENT

**4.0 ADMINISTRATIVE ITEMS****4.1 Director’s Approvals****5.0 APPROVAL OF MINUTES**

November 9, 2017

**6.0 ORAL COMMUNICATION ON ANY MATTER NOT ON THE AGENDA****7.0 COMMISSIONER’S COMMENTS**

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**COUNTY OF RIVERSIDE  
AIRPORT LAND USE COMMISSION**

**STAFF REPORT**

**AGENDA ITEM:** 3.1

**HEARING DATE:** December 14, 2017

**CASE NUMBER:** ZAP1091RI17 – Pelican Communities, Richard Hamm

**APPROVING JURISDICTION:** City of Riverside

**JURISDICTION CASE NO:** P17-0467 (General Plan Amendment), P17-0466 (Specific Plan Amendment), P17-0468 (Rezone), P17-0469 (Site Plan Review)

**MAJOR ISSUES:** None

**RECOMMENDATION:** Staff recommends that the General Plan Amendment, Specific Plan Amendment and Rezone be found CONSISTENT with the 2005 Riverside Municipal Airport Land Use Compatibility Plan, and that the Site Plan Review be found CONSISTENT, subject to the conditions included herein.

**PROJECT DESCRIPTION:** P17-0469 is a proposal to construct four three-story buildings with a total floor area of 98,608 square feet providing 108 apartment units (with clubhouse, fitness center, and leasing office) and 1,200 square feet of commercial floor area on 3.14 acres. The applicant also proposes to: (1) amend the Magnolia Avenue Specific Plan to include Mixed Use Urban (MU-U) as a General Plan land use designation in the Magnolia Center District (P17-0466); (2) amend the City's General Plan land use map designation of the site from Commercial (C) to Mixed Use Urban (MU-U) (P17-0467); and (3) rezone the site from Commercial General with Specific Plan (Magnolia Avenue) Overlay (CG-SP) to Mixed Use Urban with Specific Plan (Magnolia Avenue) Overlay (MU-U-SP) (P17-0468).

**PROJECT LOCATION:** The site is located northerly of Merrill Avenue, westerly of Riverside Avenue and easterly of De Anza Avenue, on the opposite side of Merrill Avenue from Riverside Plaza, approximately 12,930 feet northeasterly of the southeasterly terminus of Runway 9-27, and 17,200 feet northeasterly of the southerly terminus of Runway 16-34 at Riverside Municipal Airport.

**LAND USE PLAN:** 2005 Riverside Municipal Airport Land Use Compatibility Plan

- a. Airport Influence Area: Riverside Municipal Airport
- b. Land Use Policy: Airport Compatibility Zone E

c. Noise Levels: Outside the 55 CNEL contour

**BACKGROUND:**

Land Use Density/Intensity: The site is located in Airport Compatibility Zone E of the Riverside Municipal Airport Influence Area (AIA). Neither residential density nor non-residential intensity is limited within Zone E, pursuant to the Countywide Policies section of the 2004 Riverside County Airport Land Use Compatibility Plan.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zone E within the project.

Noise: The site is located outside the 55 dBA CNEL contour from Riverside Municipal Airport. Therefore, no special measures to mitigate aircraft noise are required at this location.

Part 77: While the site's distance from the southeasterly terminus of Runway 9-27 (at approximately 12,930 feet) is less than its distance from the southerly terminus of Runway 16-34 (approximately 17,200 feet), the latter point has a much lower elevation (747.5 feet above mean sea level [AMSL]) than the former point (815.8 feet AMSL). Therefore, Runway 16-34 is the applicable runway for determination of Part 77 noticing requirements at the project location. The project site elevation is 834 feet AMSL, and is proposing a maximum building height of 44 feet, which results in a top point elevation of 882 feet AMSL. At a distance of 17,200 feet from Runway 16-34, structures with a top point elevation of 919.5 feet AMSL (or greater) would require notification to the Federal Aviation Administration Obstruction Evaluation Service (FAAOES). Therefore, review of buildings by the FAA Obstruction Evaluation Services (FAAOES) was not required.

Open Area: There is no open area requirement for properties located in Compatibility Zone E.

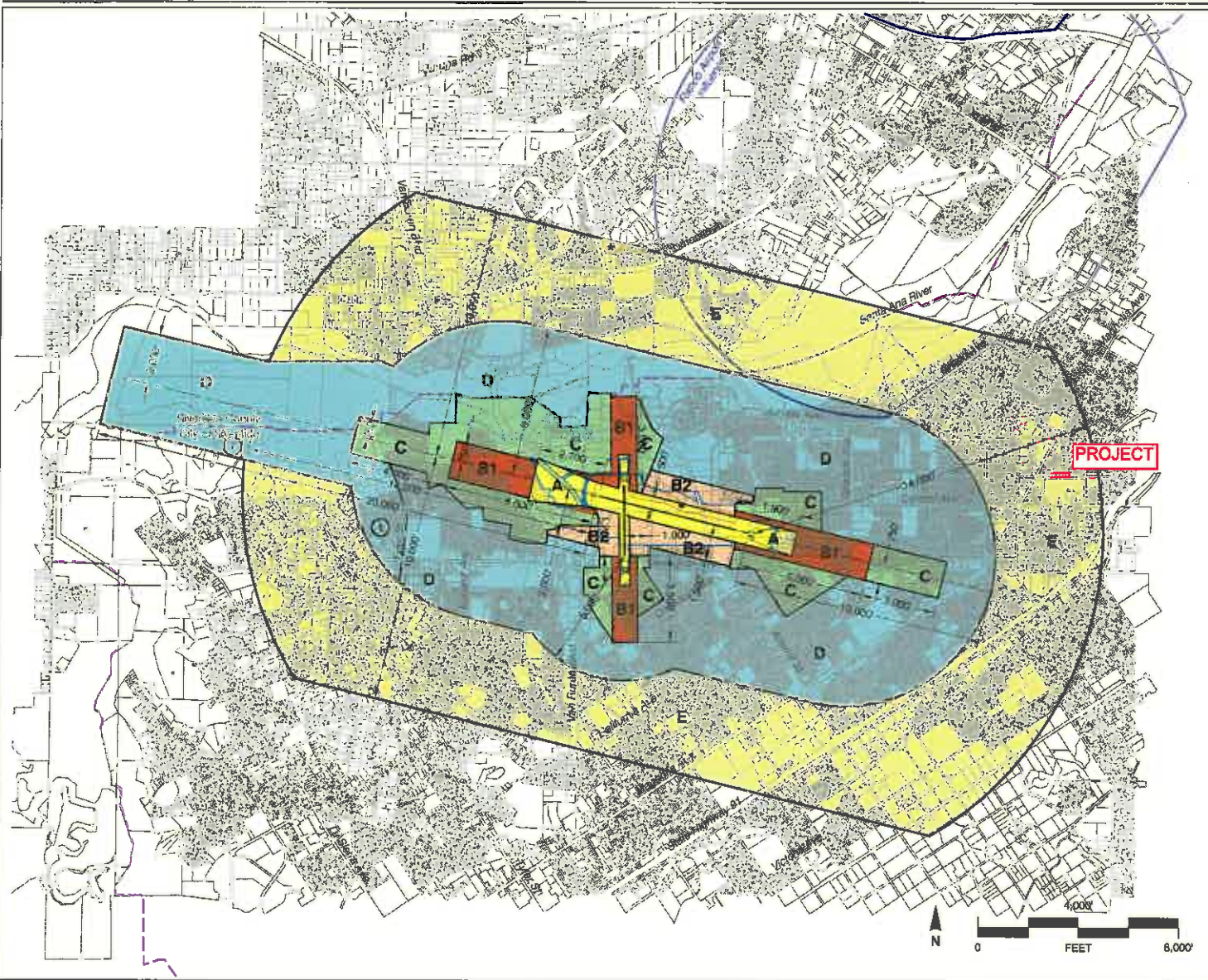
**CONDITIONS:**

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky.
2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.

- (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
- 3. The attached notice shall be given to all prospective purchasers of the property and tenants of the dwelling units to be built thereon.
- 4. Any new detention basins on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



**Legend**

**Compatibility Zones**

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E
- Height Review Overlay Zone

**Boundary Lines**

- Airport Property Line
- City Limits

**Note**

Airport influence boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

See Chapter 2, Table 2A for compatibility criteria associated with this map. See Section H1.2 for special exceptions to the Table 2A criteria.

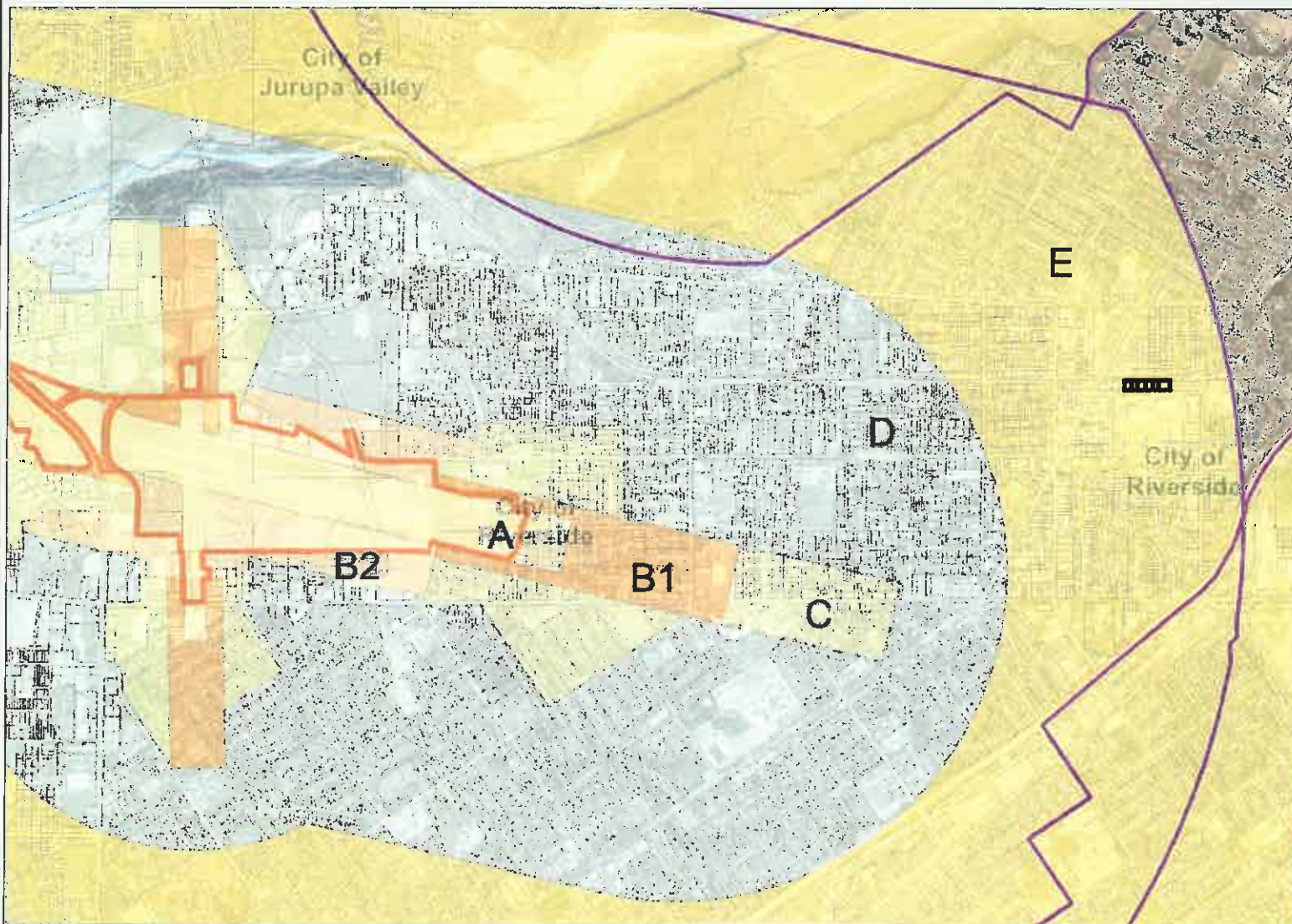
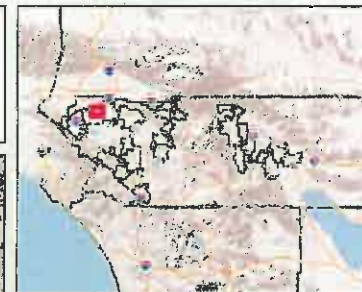
**Riverside County**  
**Airport Land Use Commission**  
**Riverside County**  
**Airport Land Use Compatibility Plan**  
**Policy Document**  
*(Adopted March 2005)*

Map RI-1

**Compatibility Map**  
**Riverside Municipal Airport**



# My Map



**Legend**

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC8
- C2-HIGHT



0 3,396 6,792 Feet



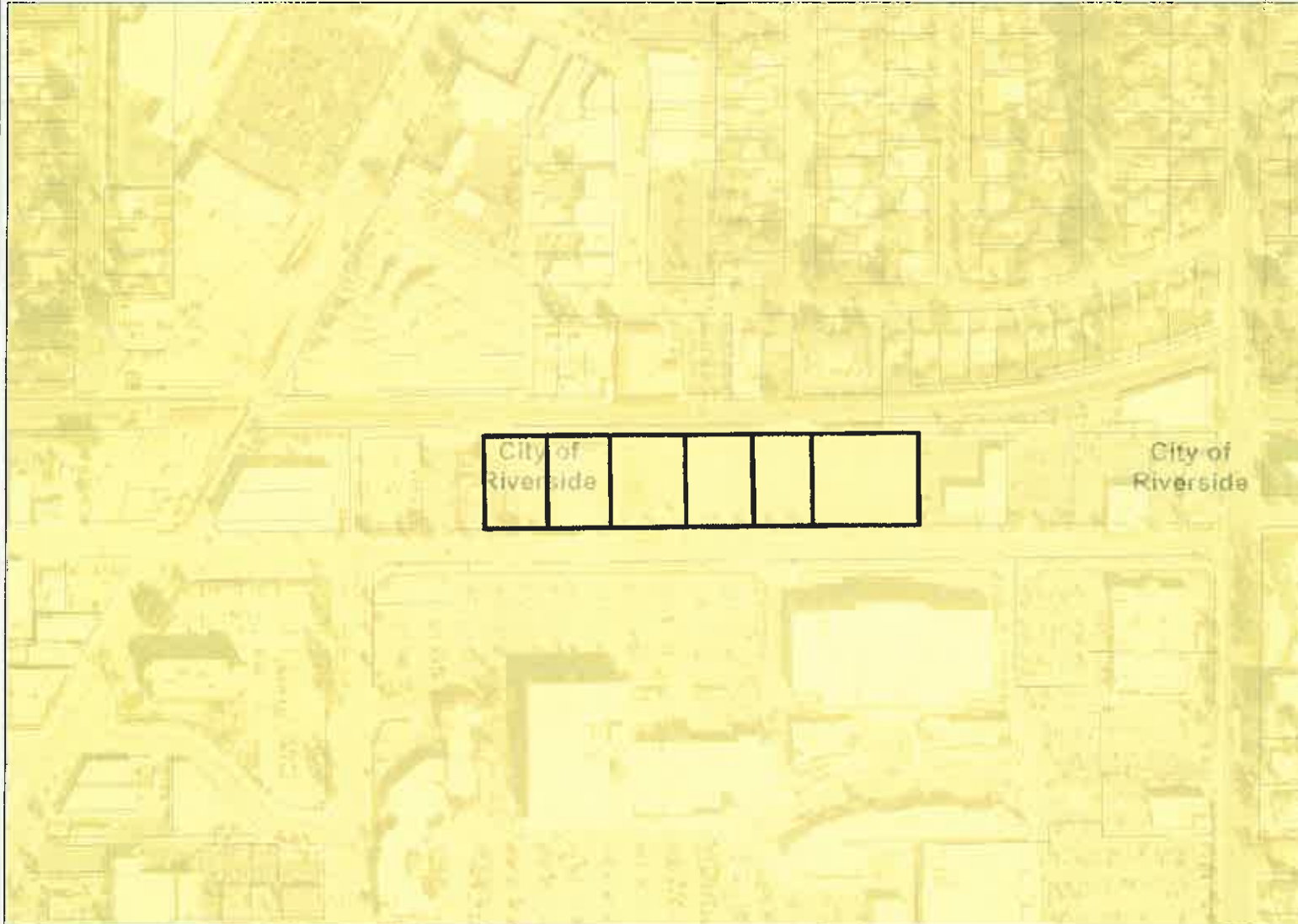
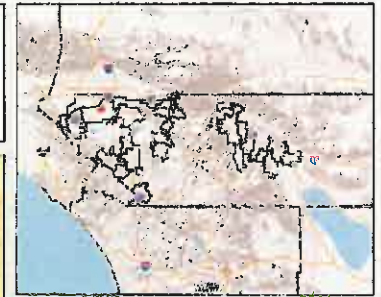
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## Notes

# My Map



## Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
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- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



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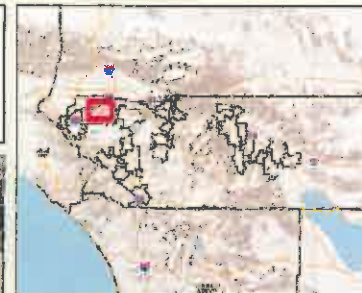
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## Notes

# My Map



## Legend

-  City Boundaries
-  Cities
- highways\_large**
  -  HWY
  -  INTERCHANGE
  -  INTERSTATE
  -  USHWY
-  majorroads
-  counties
-  cities



0 6,792 13,585 Feet



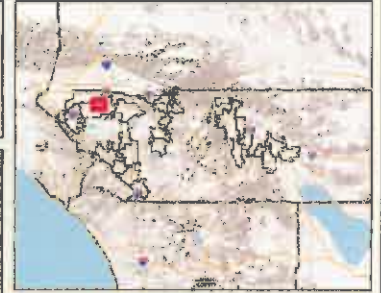
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## Notes

# My Map



## Legend

- City Boundaries
- Cities
- roadsanno
- highways
  - IHWY
  - INTERCHANGE
  - INTERSTATE
  - OFFRAMP
  - ONRAMP
  - USHWY
- roads
  - Major Roads
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- waterbodies
  - Lakes
  - Rivers



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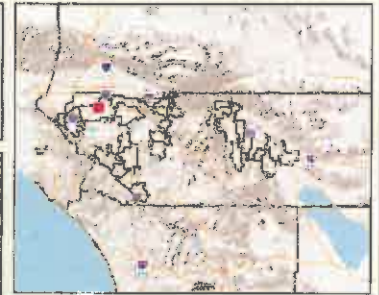
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## Notes

# My Map



## Legend

- City Boundaries
- roads
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
  - Lakes
  - Rivers



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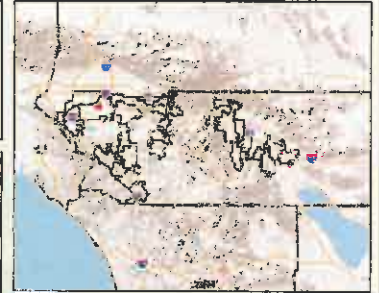
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## Notes

# My Map



## Legend

- City Boundaries
- Cities
- roadsanno
- highways
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  - INTERCHANGE
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- cities
- hydrographylines
- waterbodies
  - Lakes
  - Rivers



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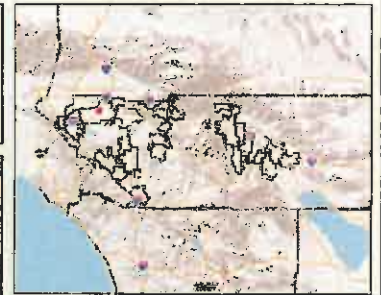
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## Notes

# My Map



## Legend

- City Boundaries
- Cities
- roadsanno
- highways
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  - INTERCHANGE
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  - OFFRAMP
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  - USHWY
- counties
- cities
- hydrographylines
- waterbodies
  - Lakes
  - Rivers



0 350 700 Feet



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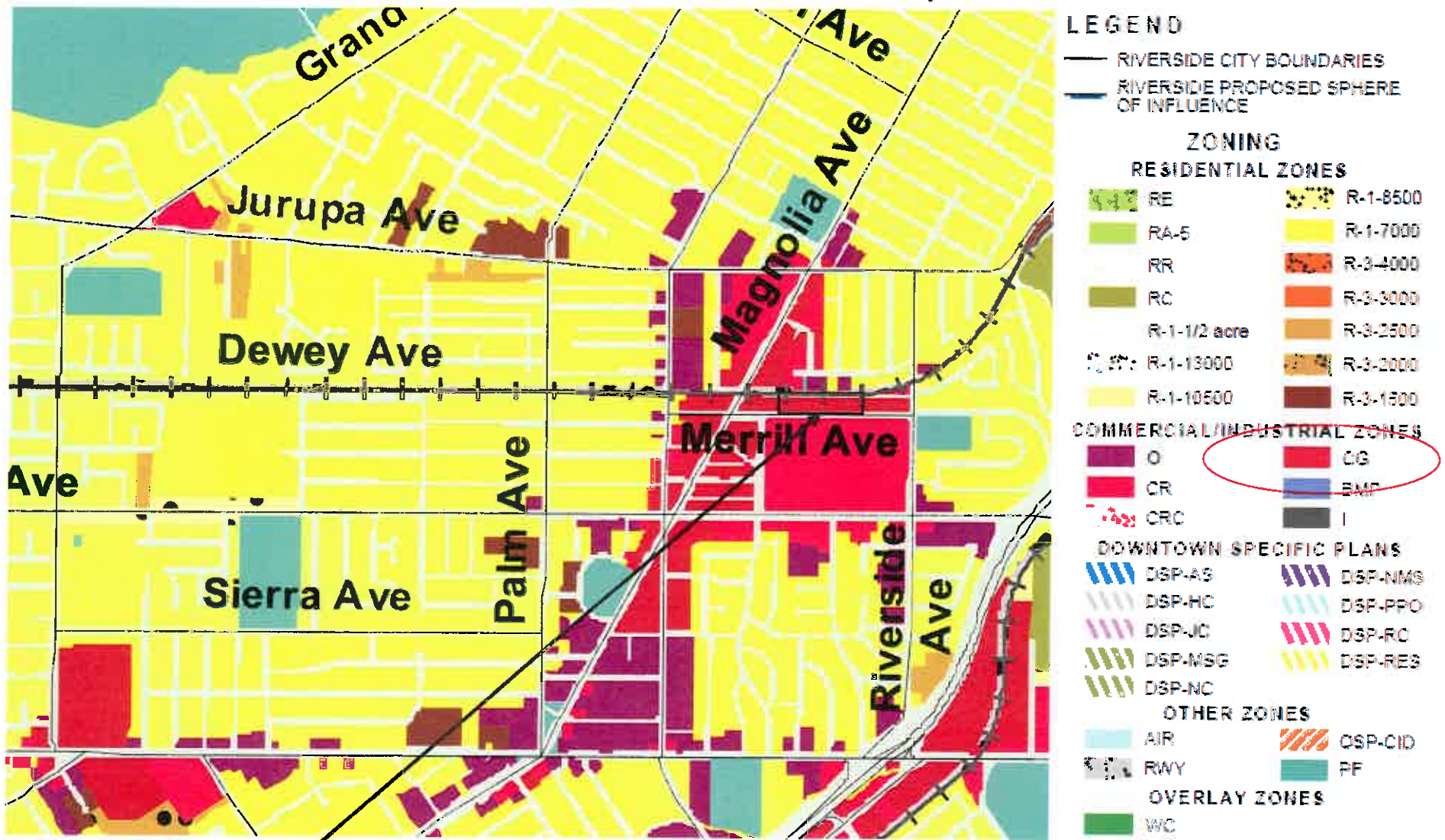
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## Notes

# Current Zoning: CG-SP

## Commercial General Zone With Specific Plan



Project Site Location



# Proposed Zoning: MU-U-SP Mixed Use Urban With Specific Plan



Project Site Location

## LEGEND

- RIVERSIDE CITY BOUNDARIES
- RIVERSIDE PROPOSED SPHERE OF INFLUENCE

### ZONING

#### RESIDENTIAL ZONES

- |              |          |
|--------------|----------|
| RE           | R-1-8500 |
| RA-5         | R-1-7000 |
| RR           | R-3-4000 |
| RC           | R-3-3000 |
| R-1-1/2 acre | R-3-2500 |
| R-1-13000    | R-3-2000 |
| R-1-10500    | R-3-1500 |

#### COMMERCIAL/INDUSTRIAL ZONES

- |     |     |
|-----|-----|
| O   | CG  |
| CR  | BMP |
| CRC | I   |

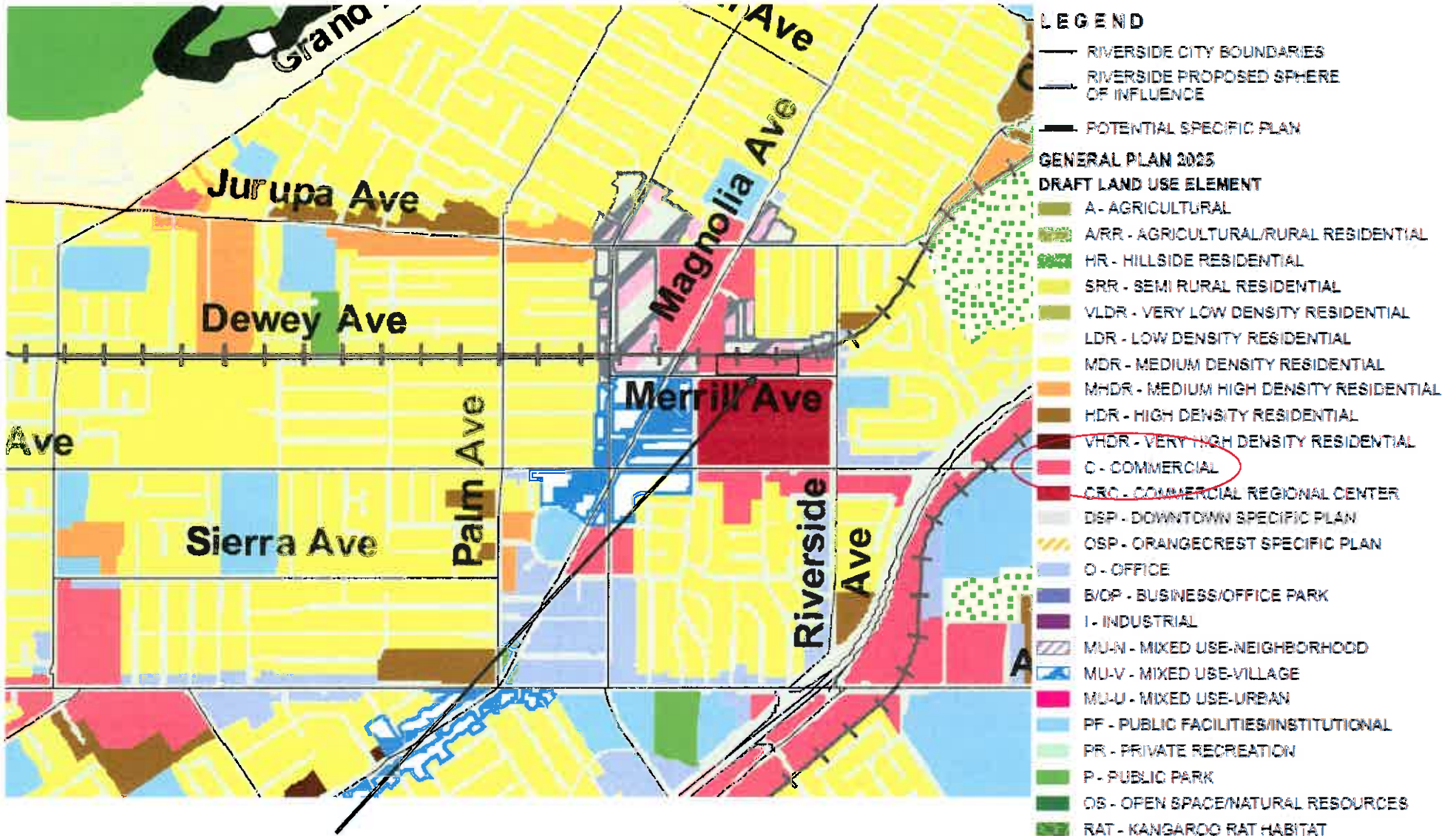
#### DOWNTOWN SPECIFIC PLANS

- |         |         |
|---------|---------|
| DSP-AS  | DSP-NMS |
| DSP-HC  | DSP-PPO |
| DSP-JC  | DSP-RC  |
| DSP-M3G | DSP-RES |
| DSP-NC  |         |

#### OTHER ZONES

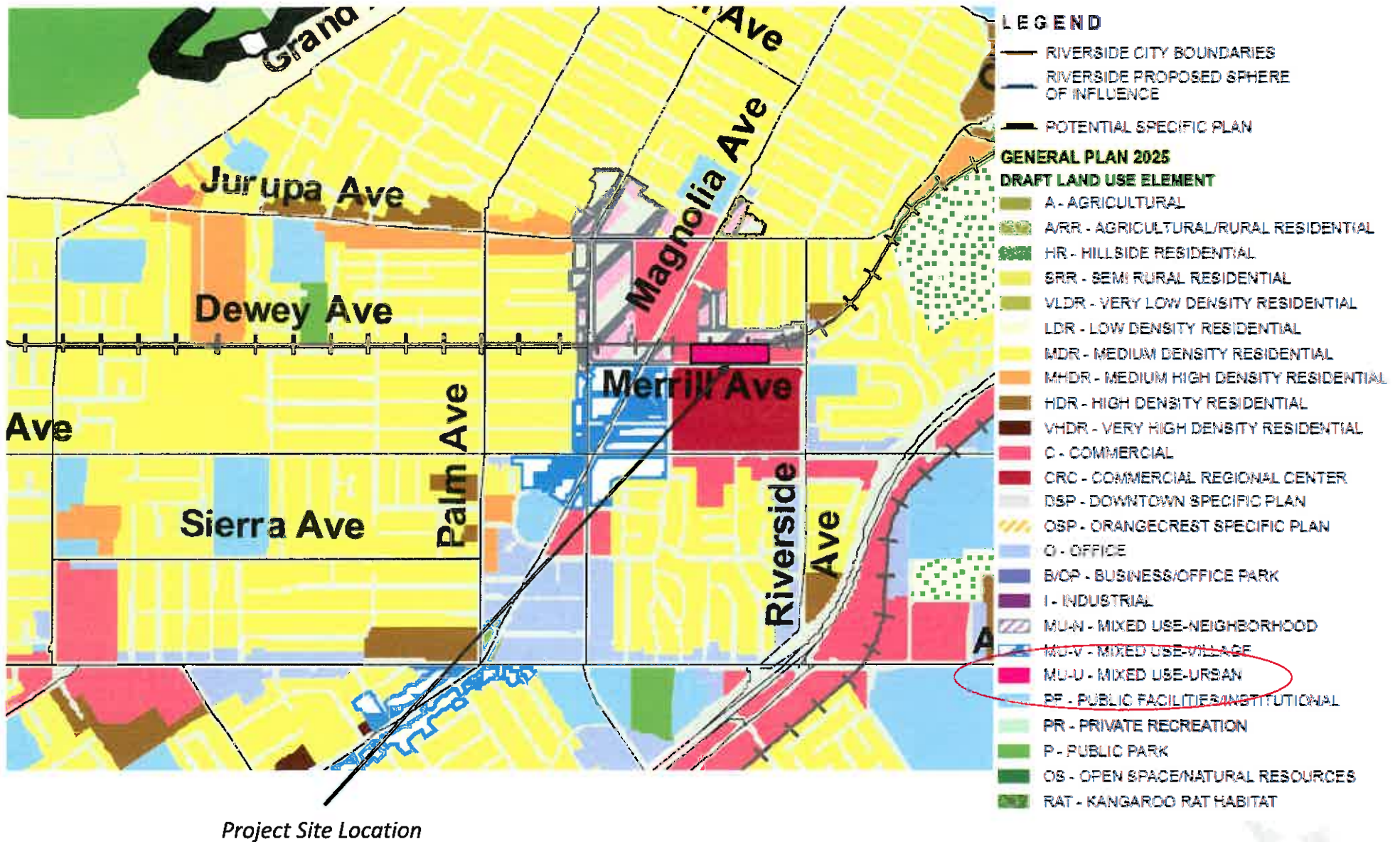
- |               |         |
|---------------|---------|
| AIR           | OSP-CID |
| RWY           | PF      |
| MU-U-SP       |         |
| OVERLAY ZONES |         |
| WC            |         |

# Current Land Use: C Commercial

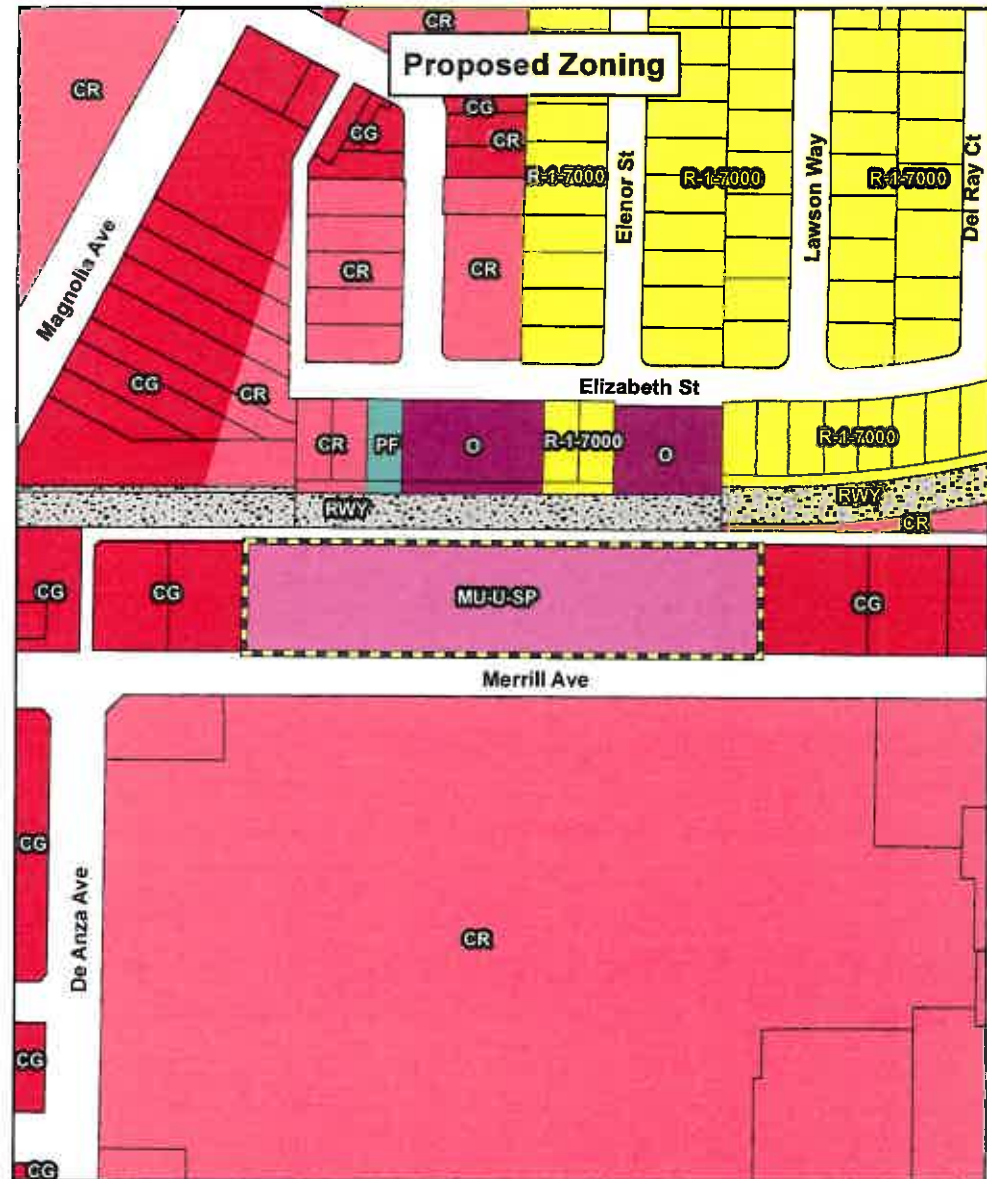
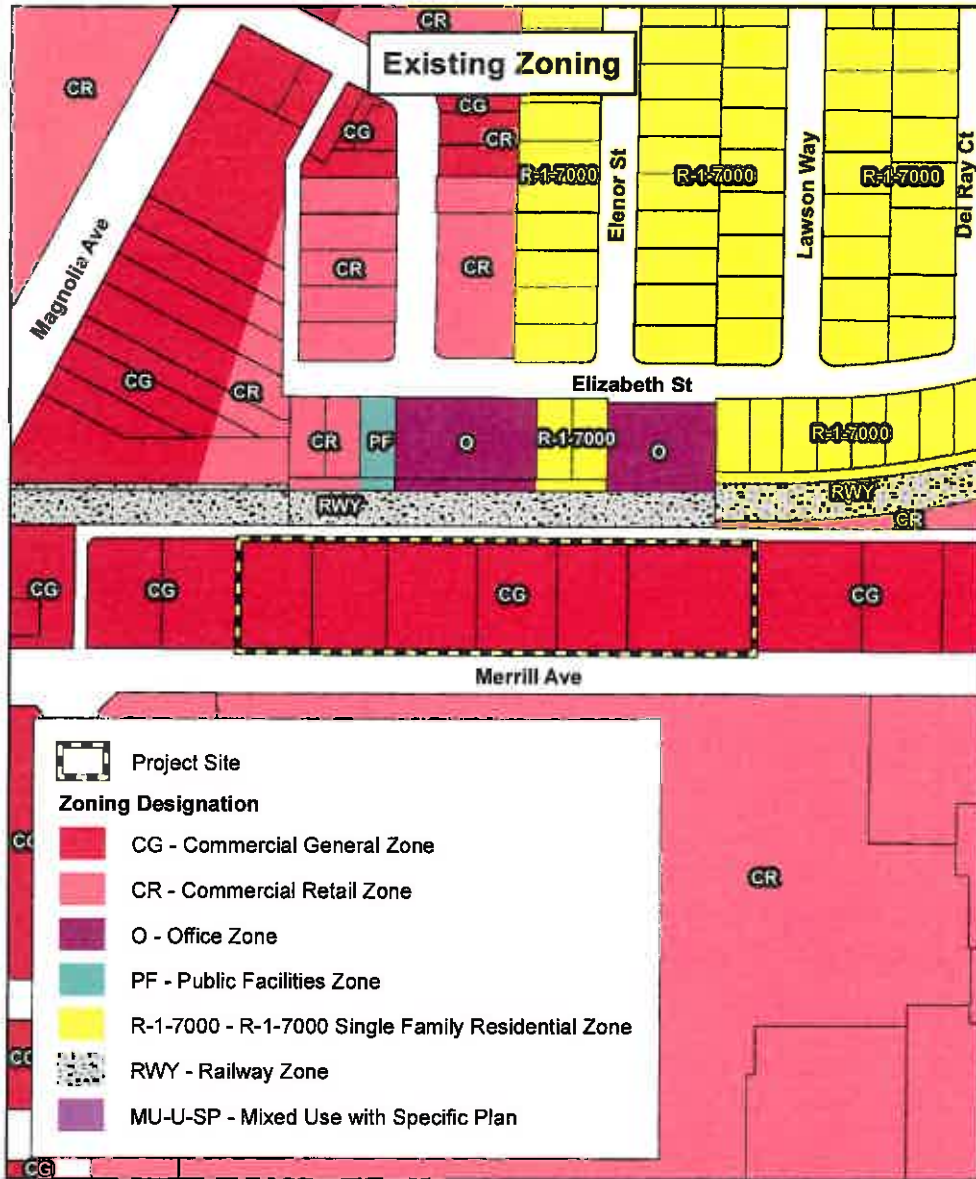


Project Site Location

# Proposed Land Use: MU-U Mixed Use-Urban



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Project Site

**Zoning Designation**

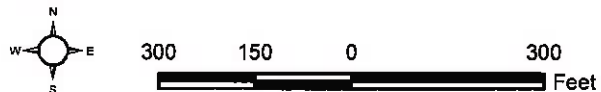
- CG - Commercial General Zone
- CR - Commercial Retail Zone
- O - Office Zone
- PF - Public Facilities Zone
- R-1-7000 - R-1-7000 Single Family Residential Zone
- RWY - Railway Zone
- MU-U-SP - Mixed Use with Specific Plan

Source: City of Riverside, 2017

### Existing and Proposed Rezone

Merrill Avenue Brownstones Project

### Exhibit 15





**PROJECT SUMMARY**

1, 2 and 3-story buildings  
 108 units  
 3,159 sqft  
 34.08 acres  
 94,208 NRSF  
 872 average NRSF  
 Amenity spaces- 3,600 SF  
 Retail (non-food service) spaces- 1,200 SF

**PARKING**

**Required (Per City Code)**  
 18 Studios x 1.5 spaces/du = 24  
 60 One Bedroom units x 1.5 spaces/du = 90  
 32 Two Bedroom units x 2.0 spaces/du = 64  
**TOTAL RESIDENTIAL PARKING REQUIRED = 178 (1.64 spaces/du)**  
**TOTAL RETAIL PARKING REQUIRED = 5 (1/250 SF)**  
**GRAND TOTAL PARKING REQUIRED = 183**

**Provided**

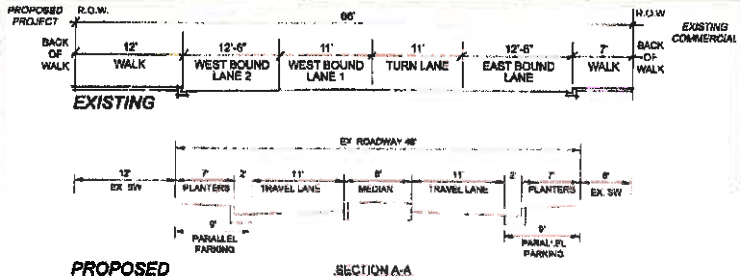
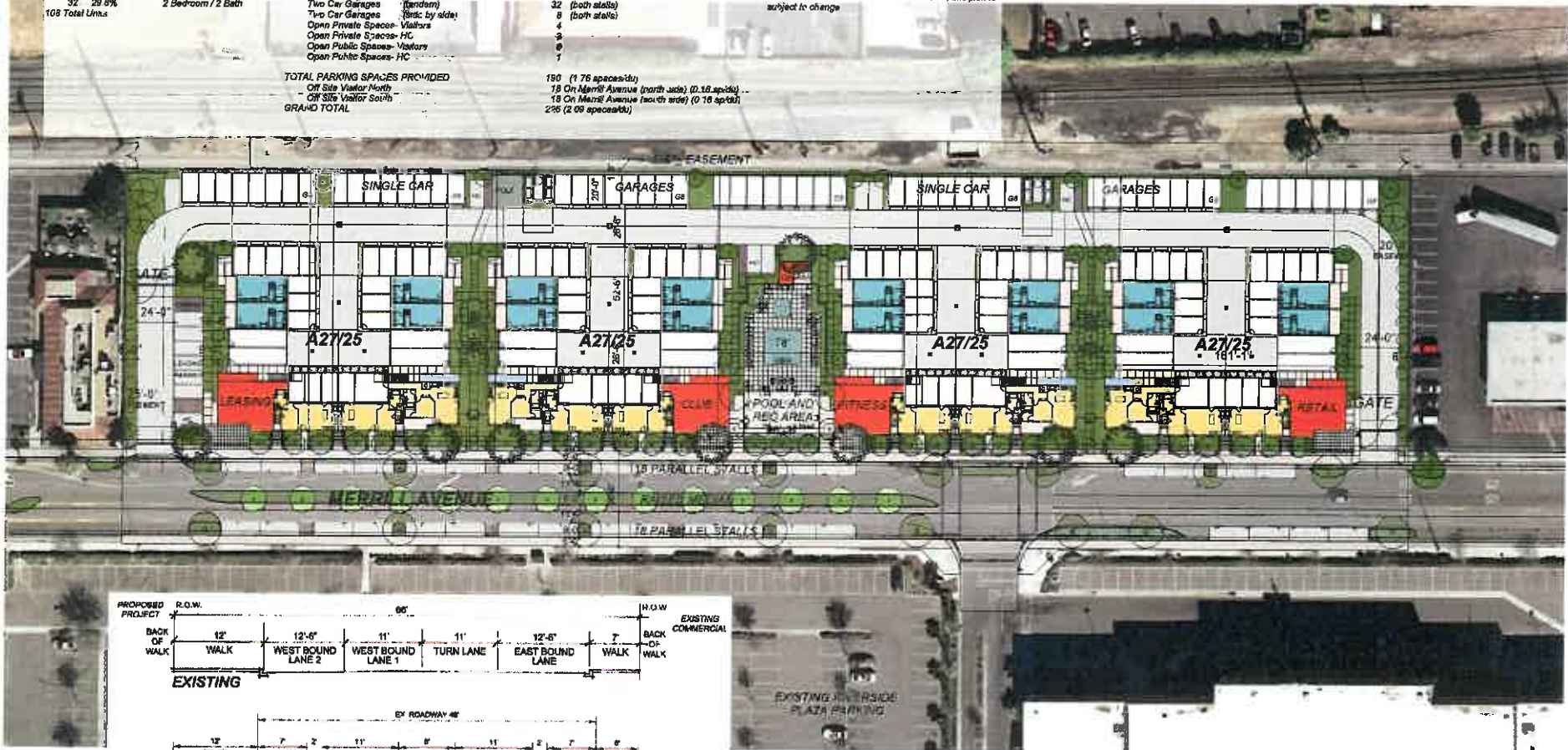
One Car Garages 56  
 One Car Garages (under apts) 80  
 Two Car Garages (random) 32 (both stalls)  
 Two Car Garages (side by side) 4 (both stalls)  
 Open Private Spaces- Visitors 4  
 Open Private Spaces- HC 4  
 Open Public Spaces- Visitors 0  
 Open Public Spaces- HC 1

**TOTAL PARKING SPACES PROVIDED**

190 (1.76 spaces/du)  
 18 On Merrill Avenue (north side) (0.18 spaces/du)  
 18 On Merrill Avenue (south side) (0.18 spaces/du)  
**GRAND TOTAL = 226 (2.09 spaces/du)**

**Notes:**

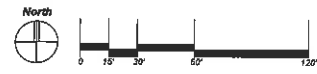
1. Parallel parking stalls are 9' x 22' minimum
2. Standard Stalls are 9' x 18'
3. Single garages are 10' x 18' clear minimum
4. Tandem garages are 10' x 36' clear minimum
5. Parking Aisles are 24'
6. Maneuvering Space behind parking stalls is 24' min
7. This Site Plan is an initial sketch and City requirements, such as, but not limited to, parking, open space (common and private), height, allowed density, setbacks, building separations, storage and fire code criteria have not been verified and therefore, this plan is subject to change



WITHOUT ALLEY

**MERRILL AVENUE BROWNSTONES** RIVERSIDE, CA

**PELICAN COMMUNITIES, LLC**  
 1900 QUAIL SLUTE 100, NEWPORT BEACH, CA 92660



**CONCEPTUAL SITE PLAN**

**ARCHITECTS ORANGE**  
 144 NORTH ORANGE ST., ORANGE, CALIFORNIA 92668 (714) 933-9900

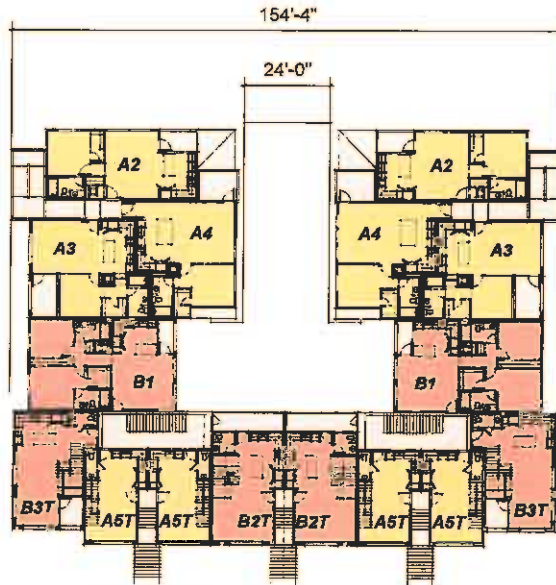
**PROGRESS**  
 2016-2017 28 SEPT 2017



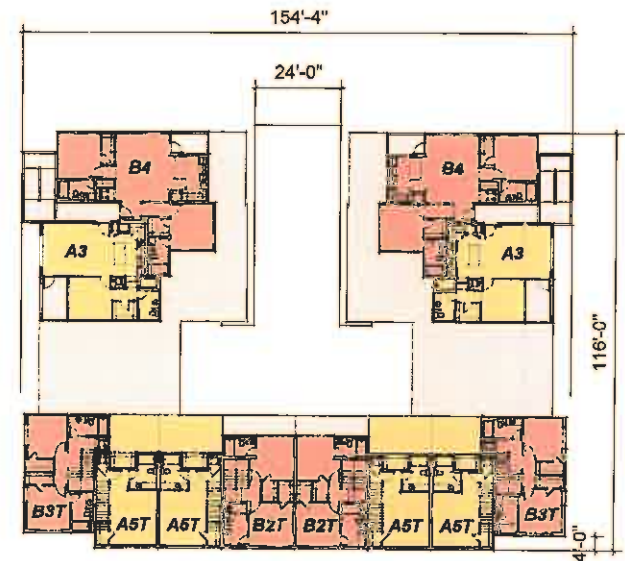
1. 2017/09/28



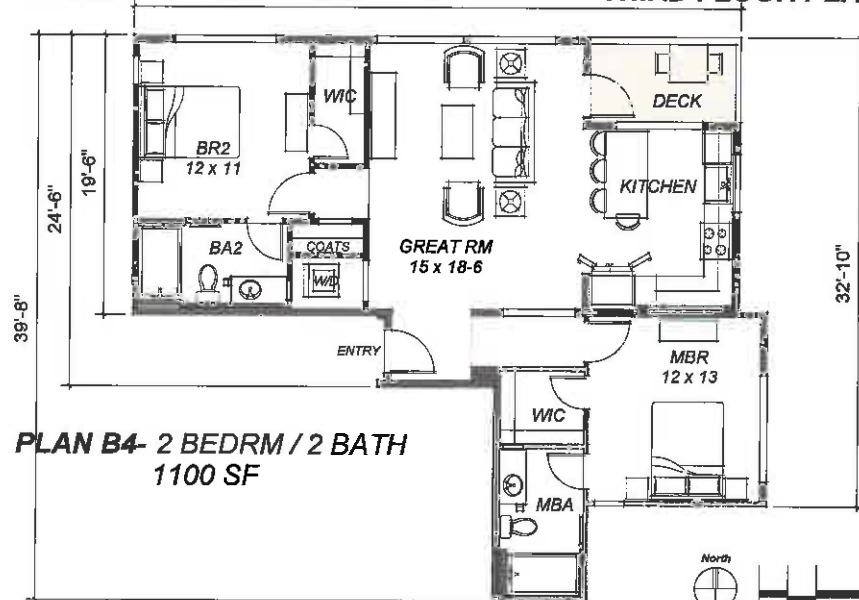
**STREET LEVEL FLOOR PLAN- 7DU/25(30s)**



**SECOND FLOOR PLAN- 16DU 42'-6"**



**THIRD FLOOR PLAN- 4DU**



**PLAN B4- 2 BEDRM / 2 BATH  
1100 SF**



**MERRILL AVENUE BROWNSTONES** RIVERSIDE, CA

**PELICAN PROPERTIES**

1500 DUAL, SUITE 100, NEWPORT BEACH, CA 92660

**A27DU/25(30s) CONCEPTUAL COMPOSITE FLOOR PLANS**

**ARCHITECTS ORANGE**  
144 NORTH ORANGE ST., ORANGE, CALIFORNIA 92665 (714) 858-9899





**SIDE ELEVATION- EAST**

LEASING OFFICE

**MATERIALS LEGEND**

- 1 EXTERIOR CEMENT PLASTER
- 2 AWNING
- 3 MANUFACTURED STONE VENEER
- 4 MANUFACTURED STONE VENEER
- 5 STOOP ENTRY ELEMENTS
- 6 WROUGHT IRON RAILING
- 7 DECORATIVE BUILDING CORNICE
- 8 BUILT UP WINDOW TRIM
- 9 MASONRY VENEER
- 10 STOREFRONT GLAZING
- 11 VINYL WINDOW FRAME (COLORED OPTION)
- 12 SIGNAGE LOCATION
- 13 GARAGE DOOR
- 14 PAINTED SHUTTERS

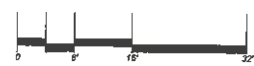


**SOUTH ELEVATION- MERRILL AVENUE**

LEASING OFFICE

**MERRILL AVENUE BROWNSTONES** RIVERSIDE, CA

PELICAN PROPERTIES  
1300 QUAL SURTE 100, NEWPORT BEACH, CA 92660



**EXTERIOR ELEVATIONS**

ARCHITECTS ORANGE  
164 NORTH ORANGE ST., ORANGE, CALIFORNIA 92668 (714) 834-9890

PROGRESS  
2015.060 29 SEPT 2017



A.28411110.DWG





MERRILL AVENUE

**SIDE ELEVATION- WEST**

STAIRS

DRIVE

GARAGES

**MATERIALS LEGEND**

- 1 EXTERIOR CEMENT PLASTER
- 2 AWNING
- 3 MANUFACTURED STONE VENEER
- 4 MANUFACTURED STONE VENEER
- 5 STOOP ENTRY ELEMENTS
- 6 WROUGHT IRON RAILING
- 7 DECORATIVE BUILDING CORNICE
- 8 BUILT UP WINDOW TRIM
- 9 MASONRY VENEER
- 10 STOREFRONT GLAZING
- 11 VINYL WINDOW FRAME (COLORED OPTION)
- 12 SIGNAGE LOCATION
- 13 GARAGE DOOR
- 14 PAINTED SHUTTERS



42'

**NORTH ELEVATION- RAILROAD TRACKS**

STAIRS

MOTORCOURT  
DRIVE

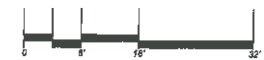
STAIRS

**MERRILL AVENUE BROWNSTONES** RIVERSIDE, CA

**PELICAN PROPERTIES**  
1300 QUAIL SLATE 100, NEWPORT BEACH, CA 92660

**EXTERIOR ELEVATIONS**

**ARCHITECTS ORANGE**  
144 NORTH ORANGE ST., ORANGE, CALIFORNIA 92665 (714) 530-9980



**PROGRESS**  
09/15/2020 09/15/2021



© 2020 Progress



**SOUTH ELEVATION- MERRILL AVENUE**

CLUB RM	RECREATION / POOL AREA	FITNESS
---------	------------------------	---------



**VIEW LOOKING NORTHEAST ON MERRILL AVENUE**

**MERRILL AVENUE BROWNSTONES** RIVERSIDE, CA

**PELICAN PROPERTIES**  
 1330 QUAIL SUITE 100, NEWPORT BEACH, CA 92660

**3D MASSING STUDIES**

**ARCHITECTS ORANGE**  
 144 NORTH ORANGE ST., ORANGE, CALIFORNIA 92676 (714) 634-6880

**PROGRESS**  
 2016-200 25 SEPT 2017

© 2016 Pelican Prop.



**NORTH ELEVATION- RAILROAD EDGE**

**GARAGES AND WALL HEIGHTS**

SEE LANDSCAPE WALL & FENCE PLAN FOR DETAILS



**VIEW LOOKING SOUTHEAST ALONG THE RAILROAD**

SEE LANDSCAPE WALL & FENCE PLAN FOR DETAILS

**MERRILL AVENUE BROWNSTONES** RIVERSIDE, CA

**PELICAN PROPERTIES**  
1300 QUAIL SLUTE 100, NEWPORT BEACH, CA 92660

**3D MASSING STUDIES**

**ARCHITECTS ORANGE**  
144 NORTH ORANGE ST., ORANGE, CALIFORNIA 92665 (714) 835-0002

**PROGRESS**

0719-340 28 SEPT 2017



© 2017 3DMS/Arch

# CONCEPTUAL GRADING PLAN

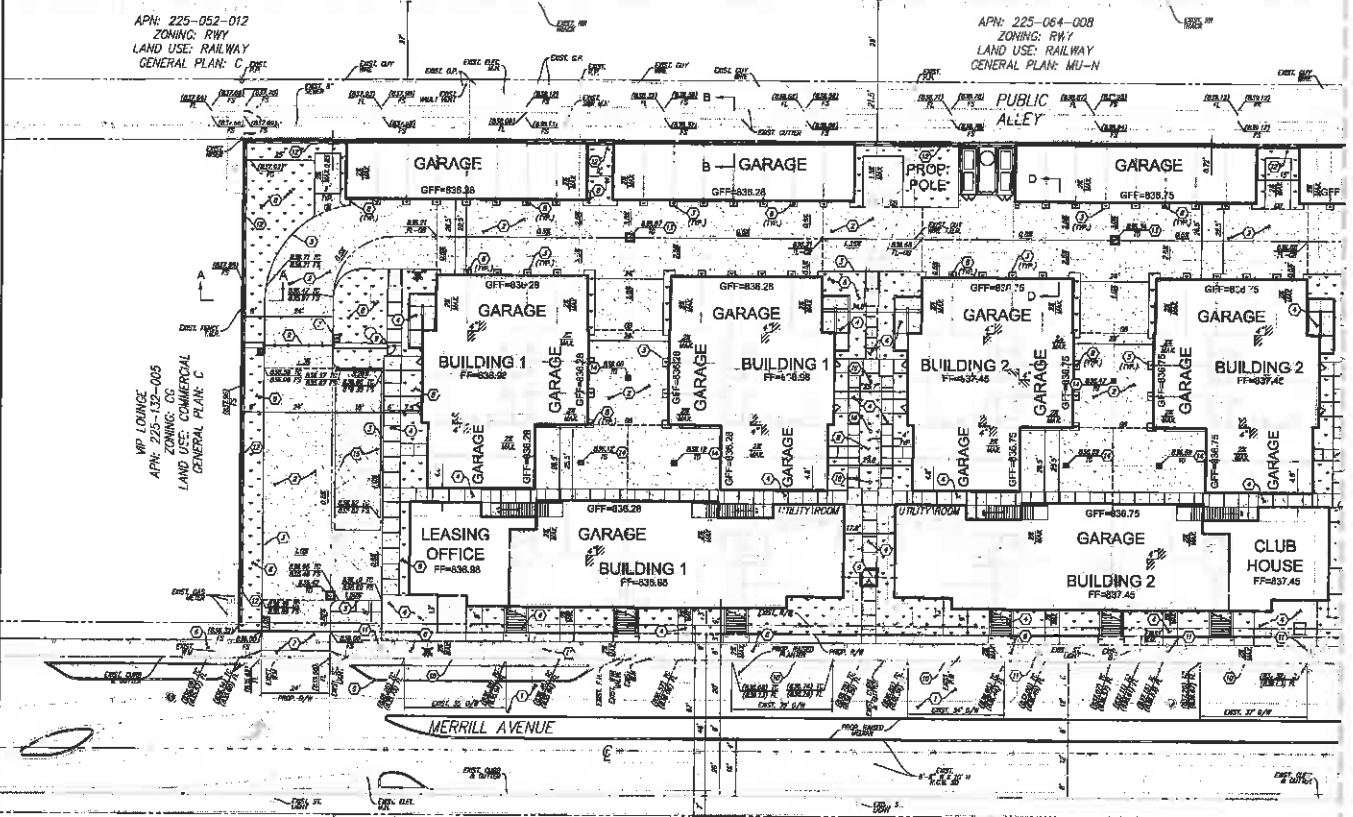
## MERRILL AVENUE BROWNSTONES

JULY 2017

APN: 225-052-012  
ZONING: RW1Y  
LAND USE: RAILWAY  
GENERAL PLAN: C

APN: 225-064-008  
ZONING: RW1  
LAND USE: RAILWAY  
GENERAL PLAN: MU-N

MP LOUNGE  
APN: 225-132-005  
ZONING: CS  
LAND USE: COMMERCIAL  
GENERAL PLAN: C



### LEGEND

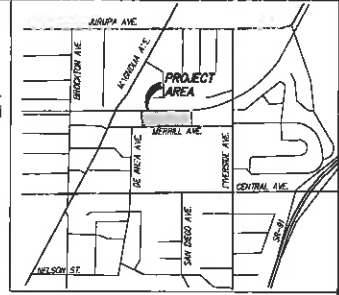
- EXISTING PROPERTY LINE
- PROPOSED RIGHT OF WAY
- EXISTING RIGHT OF WAY
- EXISTING CENTERLINE
- PROPOSED CURB
- EXISTING CURB
- PROPOSED SIDEWALK
- EXISTING SIDEWALK
- EXISTING CADASTRY
- EXISTING CONTOUR MAJOR
- EXISTING CONTOUR MINOR
- EXISTING BUILDING
- EXISTING CONCRETE
- EXISTING USE OF PAVEMENT
- EXISTING CONTOUR ELEVATION
- EXISTING SPOT ELEVATION
- CENTERLINE
- RIGHT OF WAY
- SIDEWALK
- MANHOLE
- FIRE HYDRANT
- EXISTING
- TO BE REMOVED
- GRADE BREAK
- PROPOSED
- PROP. N.T.S.
- NOT TO SCALE
- LANDSCAPE
- PROTECT IN PLACE
- REMOVE & RELOCATE
- RETAINING
- DEEP FOOTING
- GRADE INTERCEPTOR
- OFFER OF CONCERN
- SHARD POST
- STREET
- WATER METER
- RAIL BOX
- FLOOR LINE ELEVATION
- FINISH SURFACE
- FINISH GRASS
- FINISH FLOOR
- TOP OF CURB
- PAV. ELEVATION
- EXISTING GROUND
- MIN. OF PAV. ELEVATION
- SEWER
- WATER

MATCH LINE - SEE SHEET 2

- PROPOSED A.C. PAVING
- PROPOSED CONCRETE
- PROPOSED LANDSCAPE
- PROPOSED INFILTRATION CHAMBERS

### CONSTRUCTION NOTES

1. PROPOSED A.C. PAVING
2. PROPOSED CONCRETE HARDSCAPE
3. PROPOSED 6" CURB ONLY
4. PROPOSED CONCRETE S/W AND STAIRS
5. PROPOSED 8" CURB ONLY PER CITY OF RIVERSIDE STD. NO. 206
6. PROPOSED DRIVEWAY APPROACH TYPE CURB-1 PER CITY OF RIVERSIDE STANDARD NO. 302
7. PROPOSED LANDSCAPE
8. PROPOSED BROUGHN IRON GATE AND FENCE
9. EXISTING DRIVEWAY TO BE REMOVED REPLACED WITH PROPOSED CURB & GUTTER
10. EXISTING TREE TO BE REMOVED
11. PROPOSED COMBINATION RETAINING/TREE STANDING WALL
12. PROPOSED 24" X 24" BRICKS BOX
13. PROPOSED 12" X 12" BRICKS BOX
14. PROPOSED INFILTRATION CHAMBERS
15. PROPOSED 6" A TRIM GATE



VICINITY MAP  
NOT TO SCALE

**OWNER**  
PELIKAN GRANITES, LLC  
1300 OMA STREET, SUITE 100  
RIVERSIDE, CA 92507  
CONTACT: RICHARD HAHN

**ENGINEER**  
PSOMAS  
1500 IOWA AVENUE, SUITE 210  
RIVERSIDE, CA 92507  
PHONE: (951) 787-8421  
FAX: (951) 882-3378  
CONTACT: GENE CORNWELL

**UTILITY PROVIDERS**  
WATER: RPU WATER (951) 782-0239  
SEWER: CITY OF RIVERSIDE  
ELECTRICITY: RPU ELECTRIC (951) 782-0239  
GAS: THE GAS COMPANY (951) 437-2200  
TELEPHONE: AT&T (800) 888-8000  
T.V.: CHARTER COMMUNICATIONS (888) 438-2427

**ASSESSOR PARCEL NO**

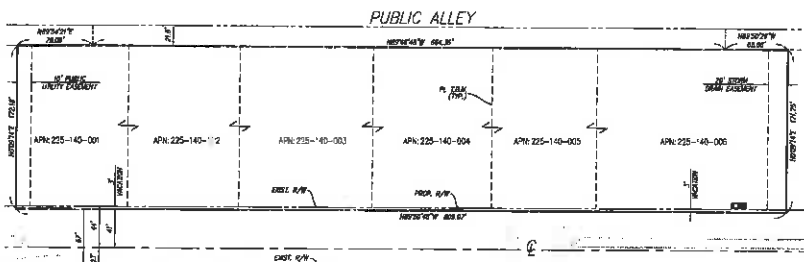
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225	140	002
225	140	003
225	140	004
225	140	005
225	140	008

**ACREAGE**

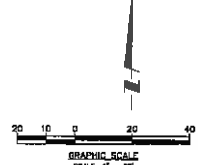
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APN: 225-140-002	0.46 ACRES
APN: 225-140-003	0.44 ACRES
APN: 225-140-004	0.43 ACRES
APN: 225-140-005	0.43 ACRES
APN: 225-140-008	0.38 ACRES
CROSS	3.14 ACRES
WATER	0.06 ACRES
NET	3.50 ACRES
DISTURBED	3.20 ACRES

**SHEET INDEX**

SHEET 1	CONCEPTUAL GRADING PLAN W/L
SHEET 2	CONCEPTUAL GRADING PLAN ELY
SHEET 3	CONCEPTUAL UTILITY PLAN



PARCEL MERCER INDEX MAP  
SCALE: 1"=50'



DATE	APPROVED	DATE	BY
<b>MERRILL AVENUE BROWNSTONES</b>			
<b>CONCEPTUAL GRADING PLAN</b>			
FOR:	PELIKAN GRANITES, LLC		
DATE:	7/2017		
DESIGNED:	OG		
CHECKED:	AM		
PLN. DR. REF:			
P.L.R.			

**PSOMAS**

1500 IOWA AVENUE, SUITE 210  
RIVERSIDE, CA 92507  
(951) 787-8421 WWW.PSOMAS.COM

SHEET 1

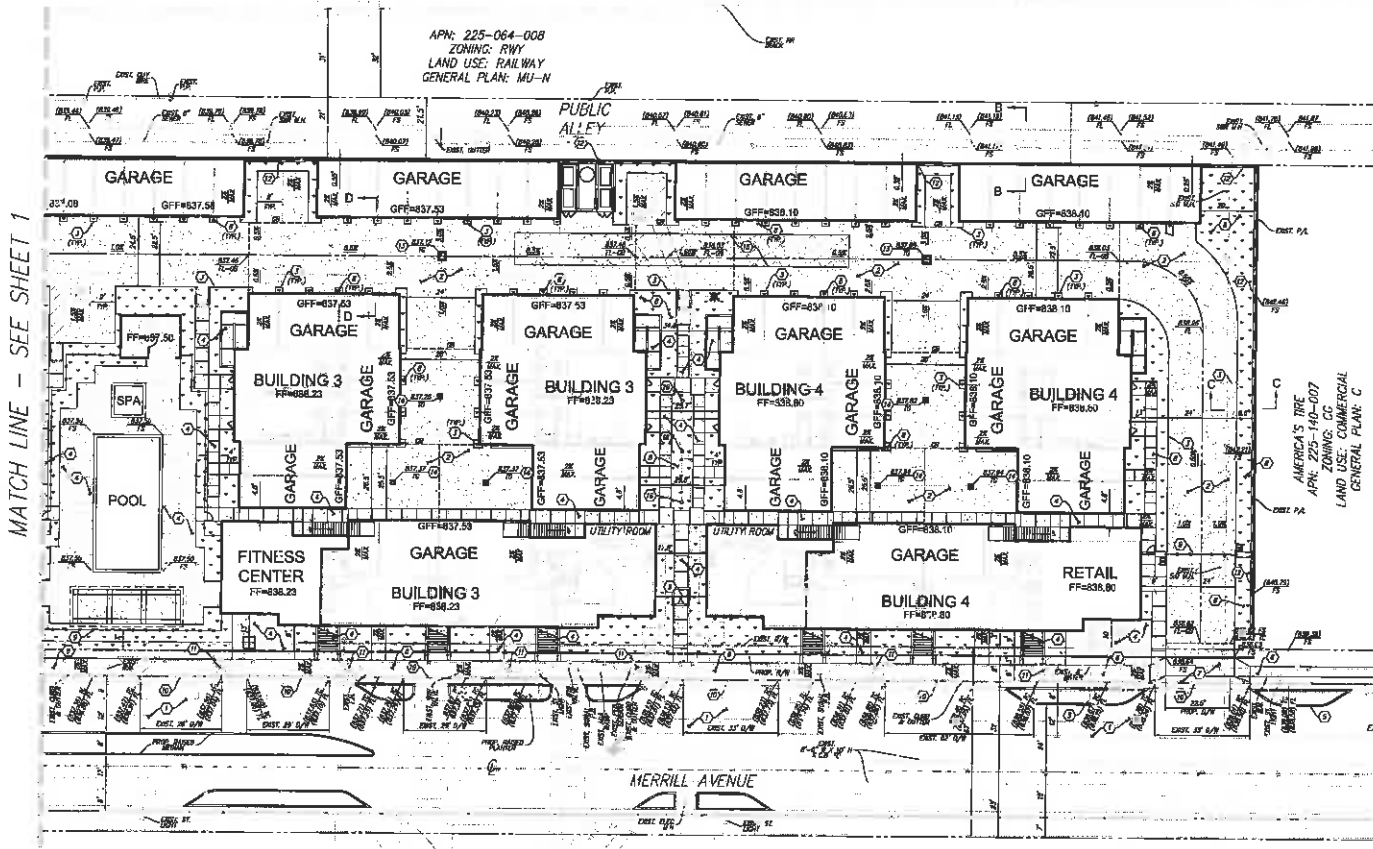
OF 3 SHEETS

DATE TSS

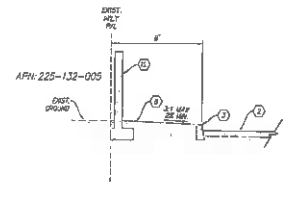
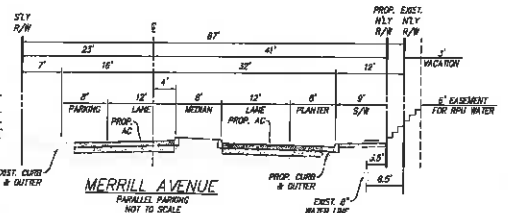
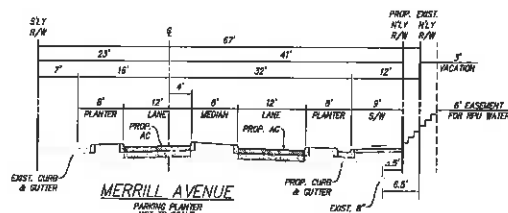
4993000101

**CONSTRUCTION NOTES**

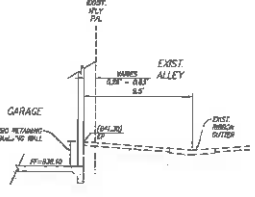
1. PROPOSED A.C. PAVING.
2. PROPOSED CONCRETE HANDBRAKE.
3. PROPOSED 6" CURB ONLY.
4. PROPOSED CONCRETE 5/8" AND STAIRS.
5. PROPOSED 6" CURB ONLY PER CITY OF RIVERSIDE STD. NO. 300, TYPE 2.
6. PROPOSED CURB SIDEWALK PER CITY OF RIVERSIDE STD. NO. 300.
7. PROPOSED DRIVEWAY APPROACH TYPE CURB-1 PER CITY OF RIVERSIDE STANDARD NO. 302.
8. PROPOSED LANDSCAPE.
9. PROPOSED Brought Iron Gate and Fence.
10. EXISTING DRIVEWAY TO BE REMOVED, REPLACED WITH PROPOSED CURB & GUTTER.
11. EXISTING TREE TO BE REMOVED.
12. PROPOSED COMBINATION RETAINING/TREE STANDING WALL.
13. PROPOSED 24" x 24" BRICKS BOB.
14. PROPOSED 12" x 12" BRICKS BOB.
15. PROPOSED REINTEGRATION CHAMBERS.
16. PROPOSED 6" TRIM GRATE.



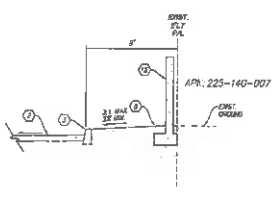
MATCH LINE - SEE SHEET 1



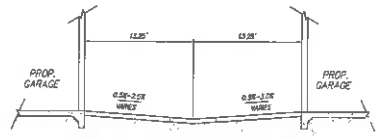
**SECTION "A-A"**  
SCALE: 1"=5'



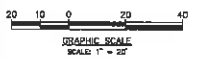
**SECTION "B-B"**  
SCALE: 1"=5'



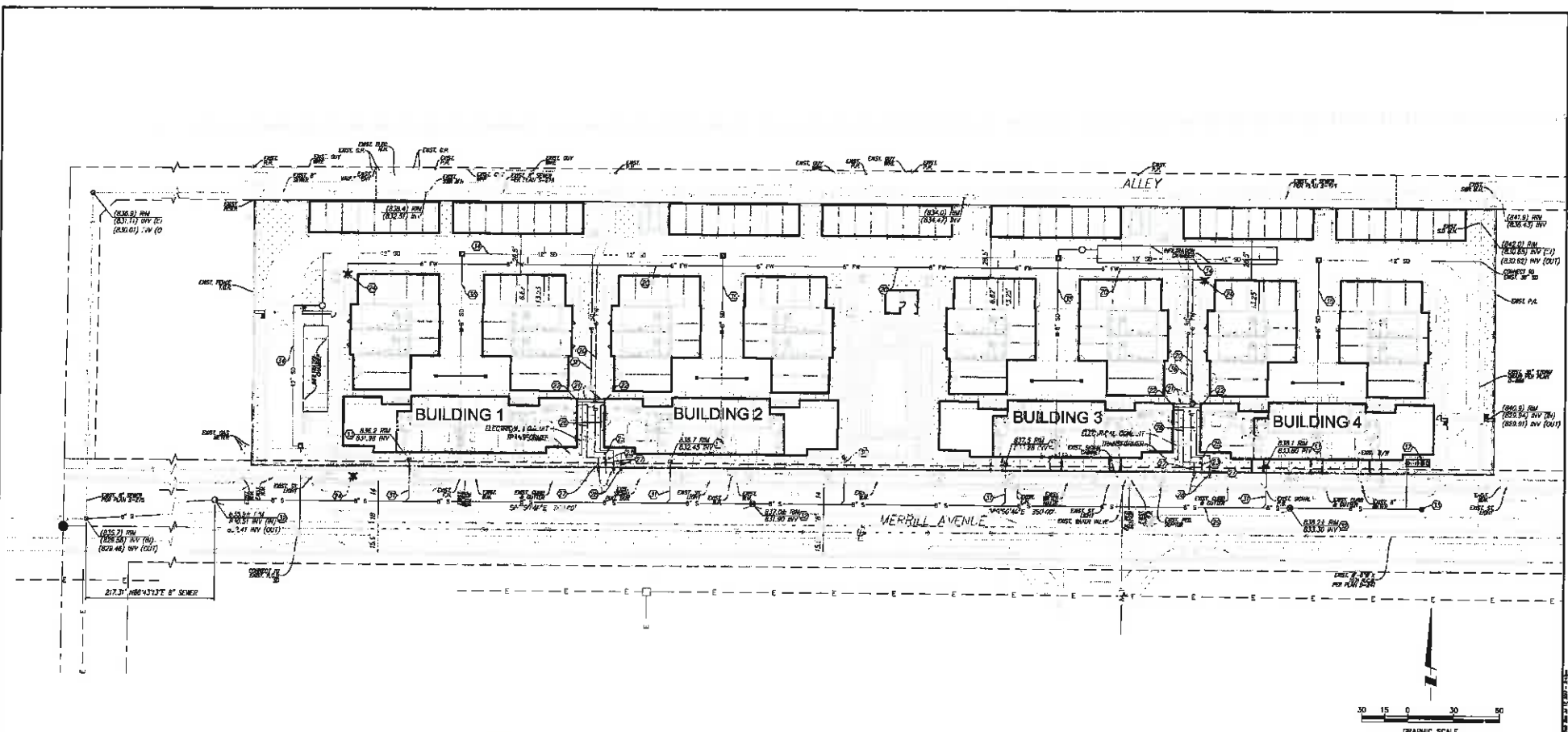
**SECTION "C-C"**  
SCALE: 1"=5'



**SECTION "D-D"**  
SCALE: 1"=5'



MERRILL AVENUE BROWNSTONES CONCEPTUAL GRADING PLAN		DATE: 07/2017	SHEET: 2
FOR: PELICAN COMMUNITIES, LLC		DESIGNED: GCS	OF: 3 SHEETS
SCALE: 1"=5'		LOCATED: 4/91	DWG. NO.
1500 IOWA AVENUE, SUITE 210		REV. OR. REF:	DESCRIPTION
RIVERSIDE, CA 92507		FB	
(951) 787-8421 WWW.PSOMAS.COM			



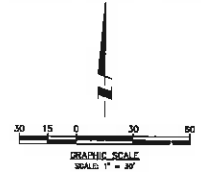
**LEGEND**

	PROPOSED AC PAVING
	PROPOSED CONCRETE
	PROPOSED LANDSCAPE

**CONSTRUCTION NOTES**

- ① PROPOSED 6" C900 DR-18 PVC PIPE
- ② PROPOSED 4" C900 DR-18 PVC PIPE
- ③ PROPOSED 24"-BUILDING RISER
- ④ PROPOSED 4" DCA
- ⑤ PROPOSED FIBRE HYDRANT
- ⑥ PROPOSED PLY. & F.D.C. ASSEMBLY
- ⑦ PROPOSED 2" WATER LINE
- ⑧ PROPOSED 2" BACKFLOW DEVICE
- ⑨ PROPOSED 2" WATER METER
- ⑩ PROPOSED 8" SDR-35 PVC SEWER PIPE
- ⑪ PROPOSED 4" SDR-35 PVC SEWER PIPE
- ⑫ PROPOSED 4" SDR-35 PVC SEWER LATERAL
- ⑬ PROPOSED SEWER MANHOLE
- ⑭ PROPOSED SEWER CLEANOUT
- ⑮ PROPOSED 12" STORM DRAIN
- ⑯ PROPOSED 8" STORM DRAIN
- ⑰ PROPOSED 6" STORM DRAIN
- ⑱ PROPOSED GREASE INTERCEPTOR

**NOTE:**  
 A STREET OPENING OR CONSTRUCTION PERMIT FROM THE CITY PUBLIC WORKS DEPT. IS REQUIRED PRIOR TO DOING ANY WORK WITHIN THE PUBLIC RIGHT OF WAY.



MERRILL AVENUE BROWNSTONES CONCEPTUAL UTILITY PLAN		SHEET 3	
PSOMAS			
1500 IOWA AVENUE, SUITE 210 RIVERSIDE, CA 92507 (951) 787-8421 WWW.PSOMAS.COM			

# NOTICE OF PUBLIC HEARING

## RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14<sup>th</sup> Floor, Riverside, California 92501, Monday through Thursday from 8:00 a.m. to 4:30 p.m., except November 23 (Thanksgiving Day), and by prescheduled appointment on Fridays, from 9:00 a.m. to 5:00 p.m., except November 24.

ATTENTION: ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. The City of Riverside will hold hearings on this project and should be contacted on non-ALUC issues.

PLACE OF HEARING: Riverside County Administration Center  
4080 Lemon St., 1<sup>st</sup> Floor Board Chambers  
Riverside, California

DATE OF HEARING: December 14, 2017

TIME OF HEARING: 9:00 A.M.

### CASE DESCRIPTION:

ZAP1091RI17 – Pelican Communities, Richard Hamm – City of Riverside Planning Case Nos. P17-0467 (General Plan Amendment), P17-0466 (Specific Plan Amendment), P17-0468 (Rezone), P17-0469 (Site Plan Review). P17-0469 is a proposal to construct four three-story buildings with a total floor area of 98,608 square feet providing 108 apartment units (with clubhouse, fitness center, and leasing office) and 1,200 square feet of commercial floor area on 3.14 acres located northerly of Merrill Avenue, westerly of Riverside Avenue and easterly of De Anza Avenue, on the opposite side of Merrill Avenue from Riverside Plaza. The applicant also proposes to: (1) amend the Magnolia Avenue Specific Plan to include Mixed Use Urban (MU-U) as a General Plan land use designation in the Magnolia Center District (P17-0466); (2) amend the City's General Plan land use map designation of the site from Commercial (C) to Mixed Use Urban (MU-U) (P17-0467), and (3) rezone the site from Commercial General with Specific Plan (Magnolia Avenue) Overlay (CG-SP) to Mixed Use Urban with Specific Plan (Magnolia Avenue) Overlay Zone (MU-U-SP) (P17-0468). (Compatibility Zone E of the Riverside Municipal Airport Influence Area).

FURTHER INFORMATION: Contact Paul Rull at (951) 955-6893. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Mr. Brian Norton of the City of Riverside Planning Division at (951) 826-2308.



# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

## APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP1091RE17 DATE SUBMITTED: 10-24-17

### APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

Applicant	Pelican Communities	Phone Number (949)263-9213
Mailing Address	1300 Quail Street, Suite 100 Newport Beach, CA 92660	Email richard.hamm@pelicancommunities.com

Representative	Richard Hamm	Phone Number (949)263-9213
Mailing Address	1300 Quail Street, Suite 100 Newport Beach, CA 92660	Email richard.hamm@pelicancommunities.com

Property Owner	City of Riverside / Redevelopment Agency City of Riverside	Phone Number (951) 826-5371
Mailing Address	3900 Main Street Riverside, CA 92522	Email BNorton@riversideca.gov

### LOCAL JURISDICTION AGENCY

Local Agency Name	Community and Economic Development Department - Planning Division	Phone Number (951)826-2308
Staff Contact	Brian Norton	Email BNorton@riversideca.gov
Mailing Address	3900 Main Street, 3rd Floor Riverside, CA 92522	Case Type
Local Agency Project No		<input checked="" type="checkbox"/> General Plan / Specific Plan Amendment <input checked="" type="checkbox"/> Zoning Ordinance Amendment <input type="checkbox"/> Subdivision Parcel Map / Tentative Tract <input type="checkbox"/> Use Permit <input checked="" type="checkbox"/> Site Plan Review/Plot Plan <input type="checkbox"/> Other

### PROJECT LOCATION

*Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways*

Street Address	3575 - 3661 Merrill Avenue Riverside, CA 92506	
Assessor's Parcel No.	225-140-001, 225-140-002, 225-140-003, 225-140-004, 225-140-005, 225-140-006	Gross Parcel Size 3.17 acres
Subdivision Name	Merrill Brownstone	Nearest Airport and distance from Airport 11,867 ft from Riverside Airport
Lot Number		

### PROJECT DESCRIPTION

*if applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed*

Existing Land Use (describe)	Vacant Lot

Riverside



Proposed Land Use (describe)	Mixed Use - 108 unit apartments & 1,200 square feet of retail	
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)	3.17 acres
For Other Land Uses (See Appendix C)	Hours of Operation	24 hours
	Number of People on Site	Maximum Number
	Method of Calculation	
Height Data	Site Elevation (above mean sea level)	840 ft.
	Height of buildings or structures (from the ground)	40 ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
	If yes, describe	

- A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:**
1. . . . . Completed ALUC Application Form
  1. . . . . ALUC fee payment
  1. . . . . Plans Package (24x36 folded) (site plans, floor plans, building elevations, landscaping plans, grading plans, subdivision maps)
  1. . . . . Plans Package (8.5x11) (site plans, floor plans, building elevations, landscaping plans, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
  1. . . . . CD with digital files of the plans (pdf)
  1. . . . . Vicinity Map (8.5x11)
  1. . . . . Detailed project description
  1. . . . . Local jurisdiction project transmittal
  3. . . . . Gummed address labels for applicant/representative/property owner/local jurisdiction planner
  3. . . . . Gummed address labels of all surrounding property owners within a 300 foot radius of the project site (**only required if the project is scheduled for a public hearing Commission meeting**). If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10) with ALUC return address. \*

\* Projects involving heliports/helicopter landing sites will require additional noticing procedures.

**COUNTY OF RIVERSIDE  
AIRPORT LAND USE COMMISSION**

**STAFF REPORT**

**AGENDA ITEM:** 3.2

**HEARING DATE:** December 14, 2017

**CASE NUMBER:** ZAP1008CO17 – Rowdy Williamson

**APPROVING JURISDICTION:** City of Corona

**JURISDICTION CASE NOS:** GPA17-001 (General Plan Amendment), CZ17-002 (Change of Zone), CUP17-004 (Conditional Use Permit)

**MAJOR ISSUES:** None

**RECOMMENDATION:** Staff recommends that the Commission find the proposed General Plan Amendment and Change of Zone CONSISTENT with the 2004 Corona Municipal Airport Land Use Compatibility Plan, and find the Conditional Use Permit CONDITIONALLY CONSISTENT, subject to the conditions included herein, and such additional conditions as may be required by the Federal Aviation Administration Obstruction Evaluation Service.

**PROJECT DESCRIPTION:** The applicant proposes to establish a 64-unit, three-story senior apartment complex on 2.2 acres. The applicant also proposes to amend the General Plan land use designation for this site from Medium Density Residential (MDR) to High Density Residential (HDR) and to change the site's zoning classification from Single Family Residential (R-1-7.2) and Low Density Residential (R-2) to High Density Multi-Family Residential (R-3).

**PROJECT LOCATION:** The site is located at 159 and 205 Buena Vista Avenue, southerly of Railroad Street, northerly of 91 Freeway, and westerly of Vicentia Avenue, in the City of Corona, approximately 8,000 feet southeasterly of the easterly terminus of Runway 7-25 at Corona Municipal Airport.

**LAND USE PLAN:** 2004 Corona Municipal Airport Land Use Compatibility Plan

- a. Airport Influence Area: Corona Municipal Airport
- b. Land Use Policy: Zone D
- c. Noise Levels: Below 55 CNEL from aircraft noise

## **BACKGROUND:**

Residential Density: The site is located within Compatibility Zone D of the Corona Municipal Airport Influence Area. Zone D criteria limit residential densities to either below 0.2 dwelling units per acre or above 5.0 dwelling units per acre. The project is proposing 64 senior apartment units on 2.2 acres, resulting in a density of 29 dwelling units per acre, which is consistent with the Compatibility Zone D criterion.

Prohibited and Discouraged Uses: The applicant does not propose any uses specifically prohibited or discouraged in Compatibility Zone D of the Corona Municipal Airport Influence Area (highly noise-sensitive outdoor nonresidential uses and hazards to flights).

Noise: The site is located outside the 55 dBA CNEL contour from Corona Municipal Airport. ALUC's objective is that residential interior noise levels from aviation-related sources within the Corona Municipal Airport Influence Area not exceed CNEL 45 dB. As standard construction for new homes is presumed to provide adequate sound attenuation where the exterior noise exposure is not more than 20 dB greater than the interior standard, this residential development would not require special measures to mitigate aircraft-generated noise.

Part 77: The distance from the easterly terminus of Runway 7-25 at Corona Municipal Airport to the nearest point of the project site is approximately 8,000 feet. The elevation of Runway 7-25 at its easterly terminus is 533 feet above mean sea level. At this distance, structures with a top point elevation of 613 feet above mean sea level (or greater) would require notification to the Federal Aviation Administration Obstruction Evaluation Service (FAAOES). The site finished floor elevation is 612 feet above sea mean level and the project proposes a maximum building height of 38.5 feet, for a total maximum elevation of 650.5 feet above mean sea level. Therefore, notification to the FAAOES for height reasons is required. The applicant has submitted Form 7460-1. FAA OES has assigned Aeronautical Study No. 2017-AWP-11657-OE to this project, and its status is currently "work in progress".

Open Area: Compatibility Zone D requires that 10% of the area within major projects (10 acres or larger) be set aside as open land that could potentially serve as emergency landing areas. Due to the project's size (2.2 acres) being lower than the 10 acre threshold, open space set-asides are not required.

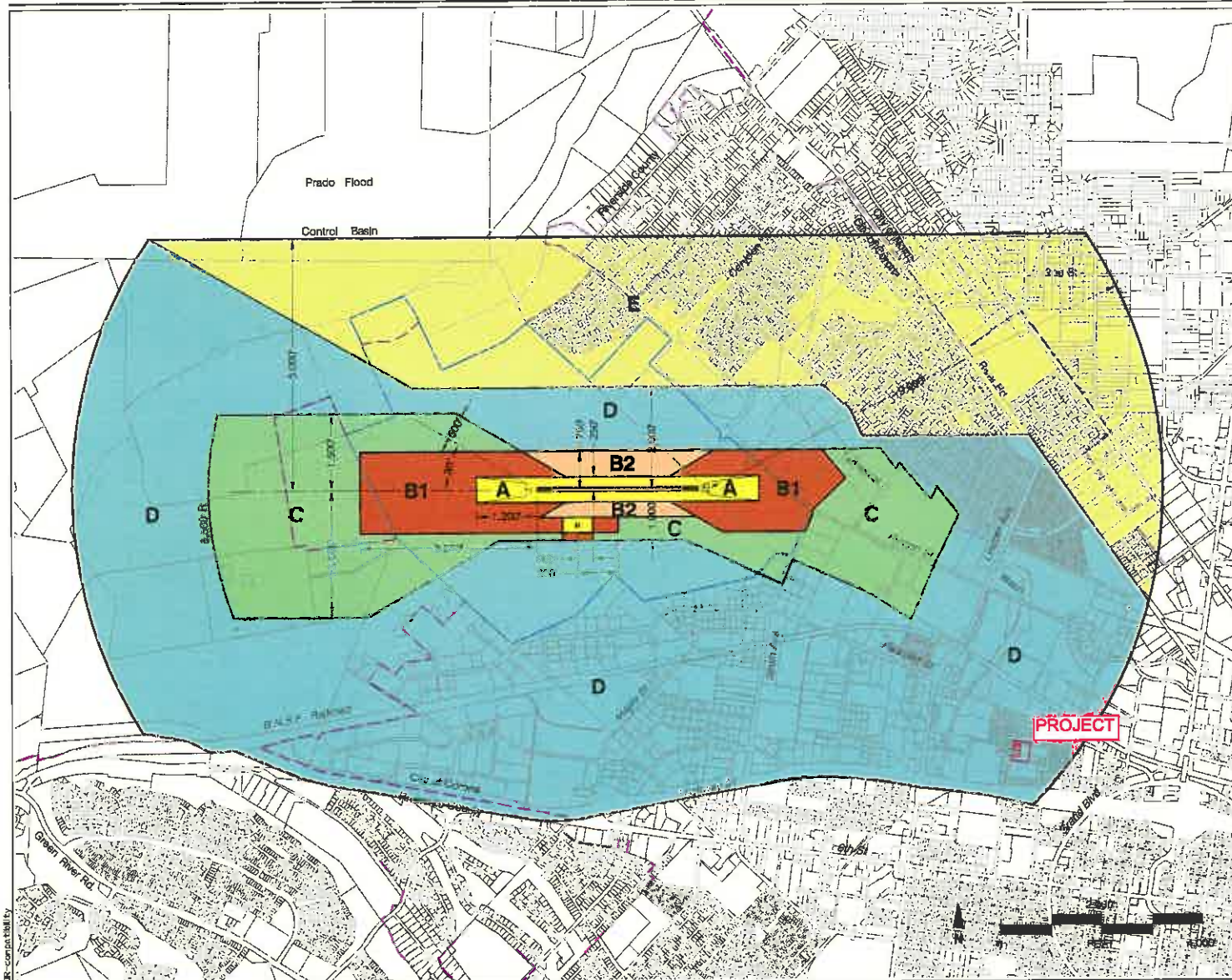
## **CONDITIONS:**

1. Any outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:

- (a) Any activity which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any activity which would cause sunlight to be reflected toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any activity which would generate smoke or water vapor, or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, composting operations, production of cereal grains, sunflower, and row crops, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
  - (d) Any activity which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Highly noise-sensitive outdoor nonresidential uses.
3. The attached disclosure notice shall be provided to all potential purchasers and tenants of the proposed dwelling units, and shall be recorded as a deed notice.
  4. Any ground-level or aboveground water detention basin or facilities shall be designed so as to provide for a detention period for the design storm that does not exceed 48 hours and to remain totally dry between rainfalls. Vegetation in and around such facilities that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
  5. Prior to issuance of building permits for any structures, the permittee (or its successor-in-interest) shall submit evidence to the City of Corona Development Services Department that the Federal Aviation Administration (FAA) has issued a determination of "Not a Hazard to Air Navigation" for such structure.








# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



**Legend**

**Compatibility Zones**

-  Airport Influence Area Boundary
-  Zone A
-  Zone B1
-  Zone B2
-  Zone C
-  Zone D
-  Zone E

**Boundary Lines**

-  Airport Property Line
-  City Limits

**Note**

Airport Influence boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

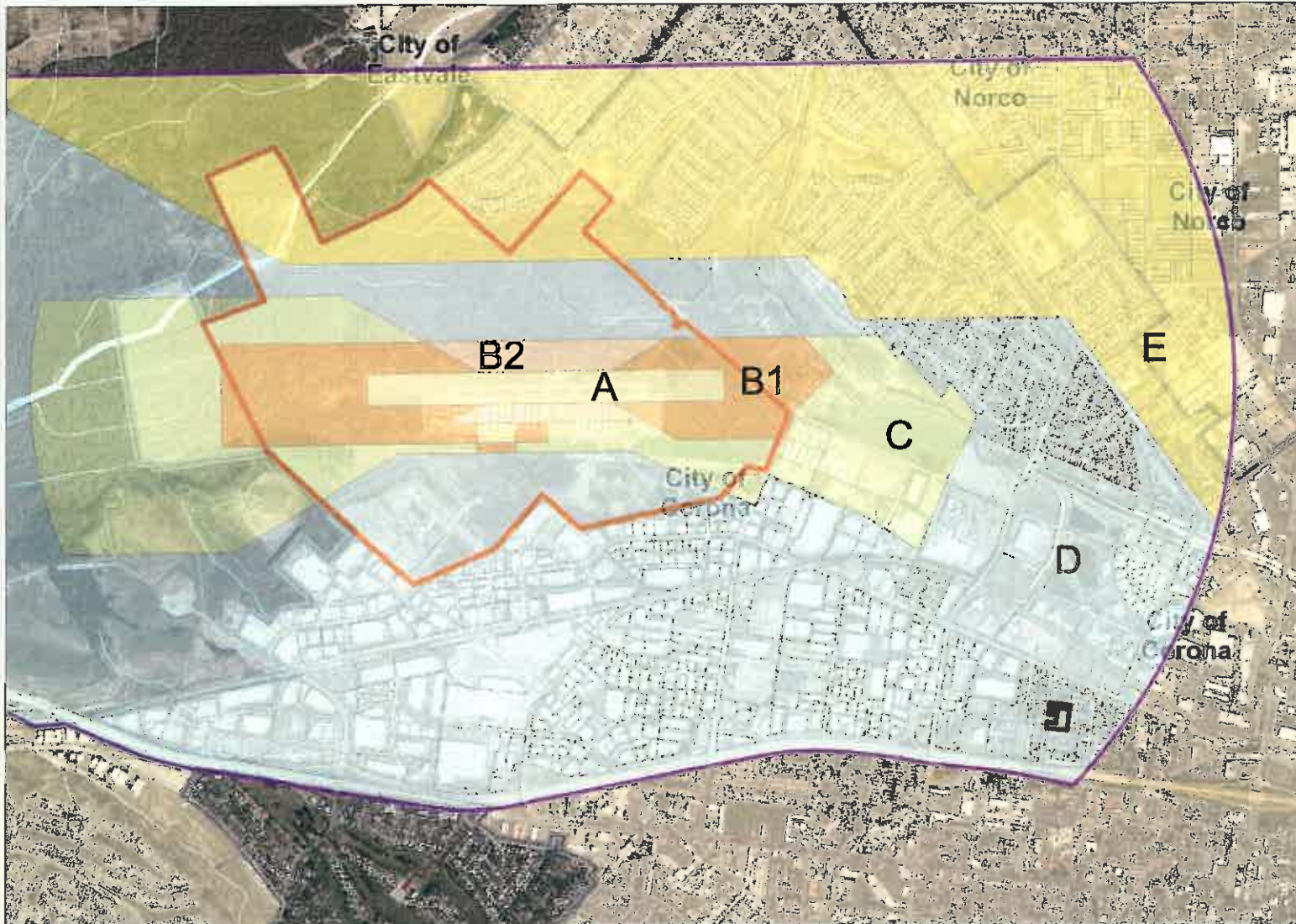
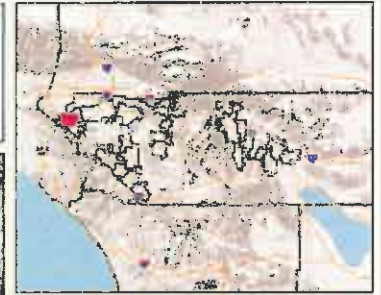
See Chapter 2, Table 2A for compatibility criteria associated with this map.

Riverside County  
 Airport Land Use Commission  
**Riverside County  
 Airport Land Use Compatibility Plan  
 Policy Document**  
 (Adopted October 2004)

Map CO-1

**Compatibility Map**  
 Corona Municipal Airport

# My Map



## Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



0 2,933 5,865 Feet



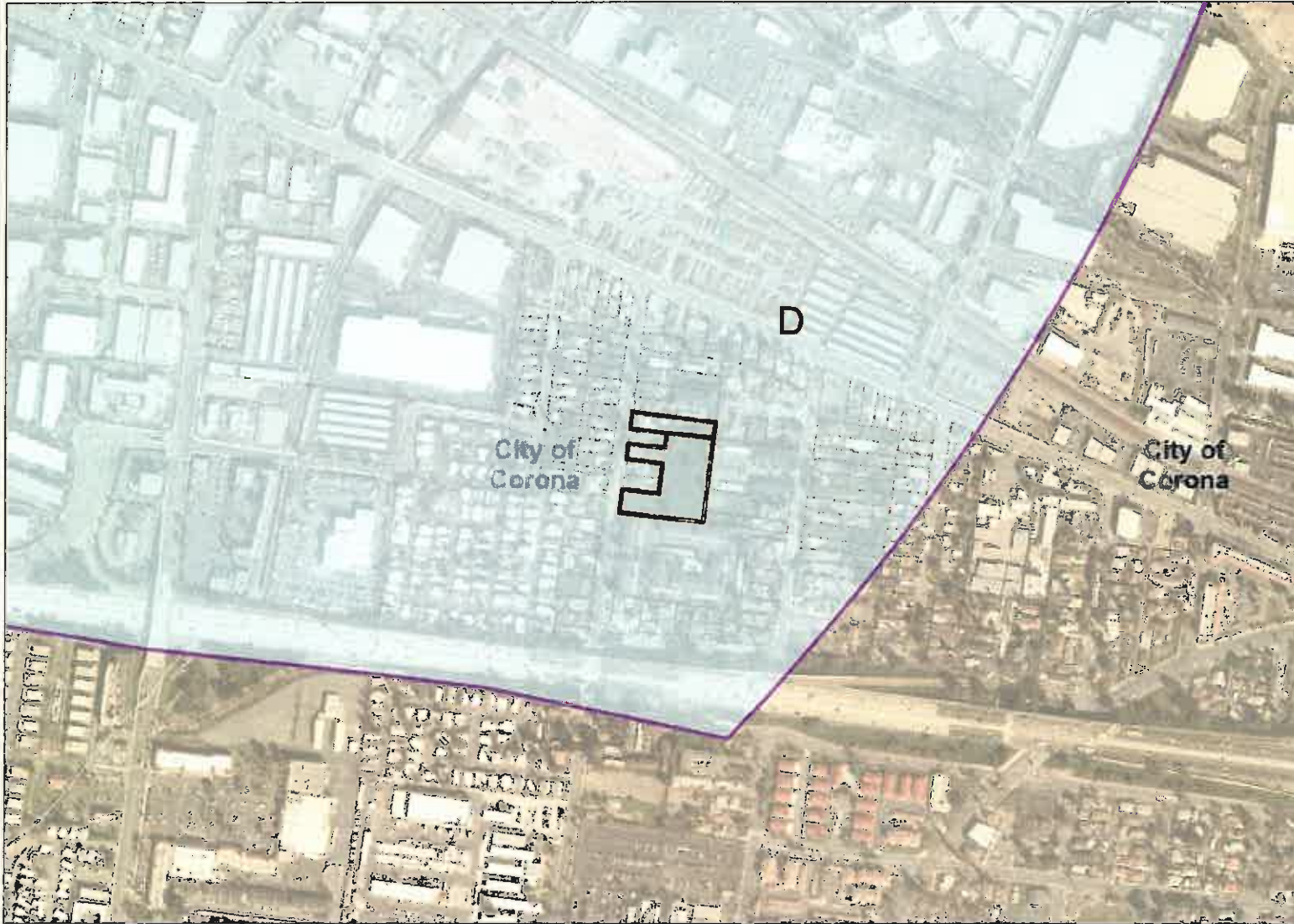
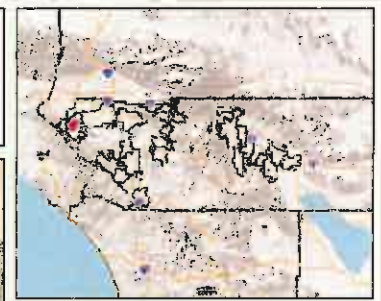
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## Notes

# My Map



## Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



0 733 1,466 Feet



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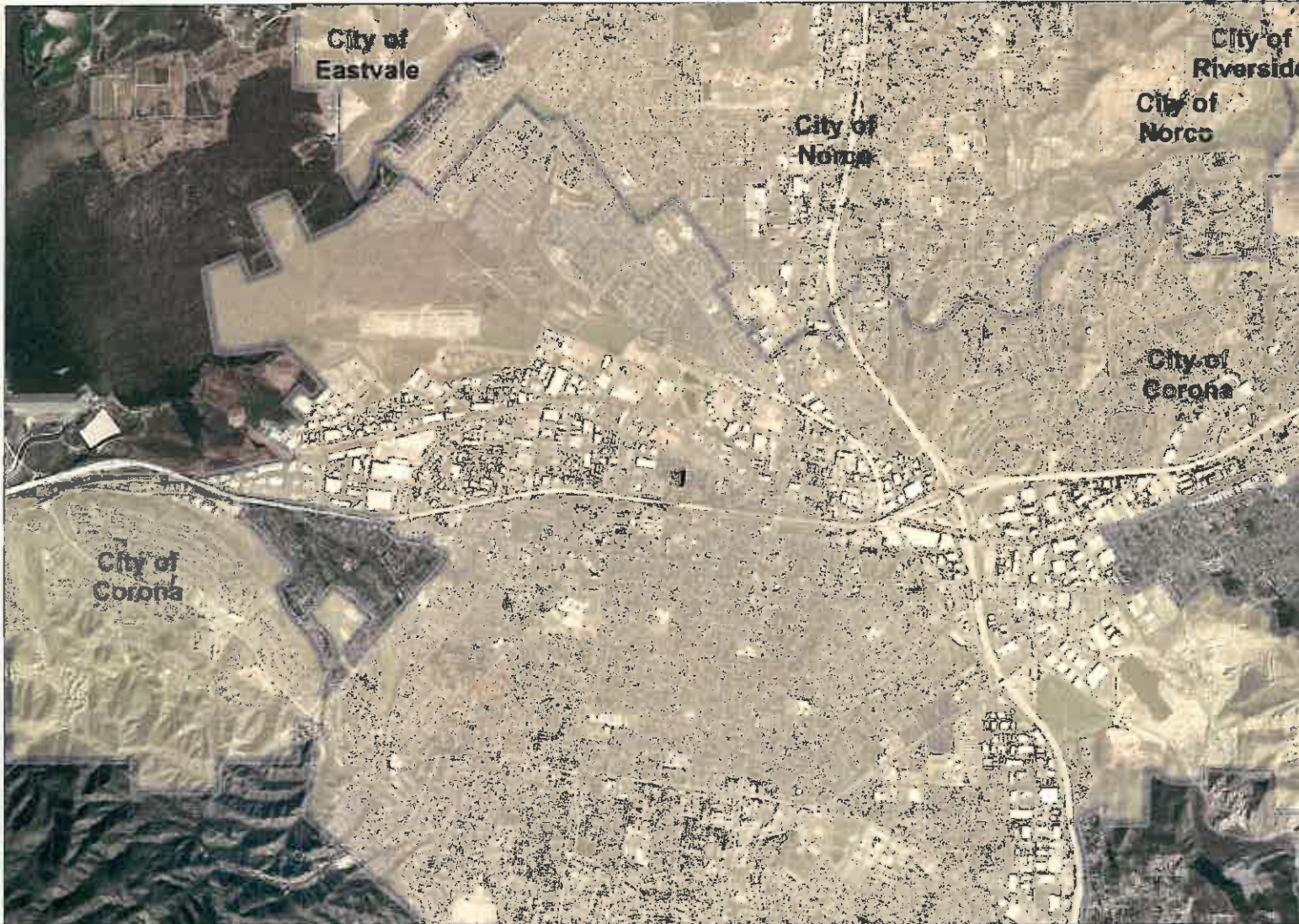
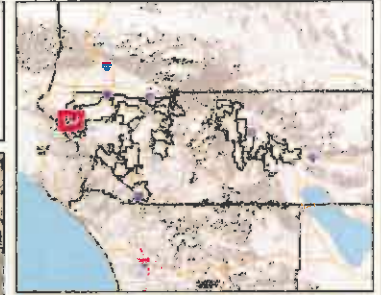
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## Notes



# My Map



## Legend

-  City Boundaries
-  Cities
- highways\_large**
-  HWY
-  INTERCHANGE
-  INTERSTATE
-  USHWY
-  majorroads
-  counties
-  cities
- hydrographylines**
- waterbodies**
-  Lakes
-  Rivers



0 5,865 11,730 Feet



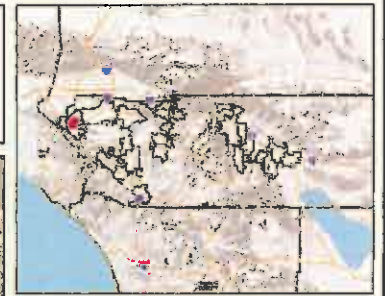
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## Notes

# My Map



## Legend

- City Boundaries
- Cities
- roadsanno
- highways
  - HWY
  - INTERCHANGE
  - INTERSTATE
  - OFFRAMP
  - ONRAMP
  - USHWY
- counties
- cities
- hydrographylines
- waterbodies
  - Lakes
  - Rivers



0 1,466 2,933 Feet



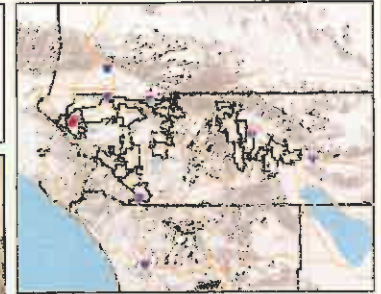
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## Notes

# My Map



## Legend

- City Boundaries
- Cities
- roadsanno
- highways
  - HWY
  - INTERCHANGE
  - INTERSTATE
  - OFFRAMP
  - ONRAMP
  - USHWY
- counties
- cities
- hydrographylines
- waterbodies
  - Lakes
  - Rivers



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## Notes

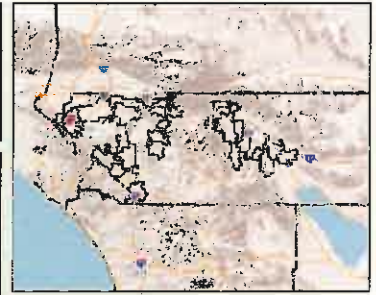
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# My Map



### Legend

- City Boundaries
- Cities
- roadsanno
- highways
  - HWY
  - INTERCHANGE
  - INTERSTATE
  - OFFRAMP
  - ONRAMP
  - USHWY
- counties
- cities
- hydrographylines
- waterbodies
  - Lakes
  - Rivers



0 367 733 Feet



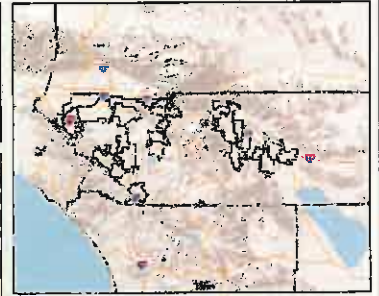
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### Notes

# My Map



## Legend

- City Boundaries
- Cities
- roadsanno
- highways
  - HWY
  - INTERCHANGE
  - INTERSTATE
  - OFFRAMP
  - ONRAMP
  - USHWY
- counties
- cities
- hydrographylines
- waterbodies
  - Lakes
  - Rivers



0 183 367 Feet



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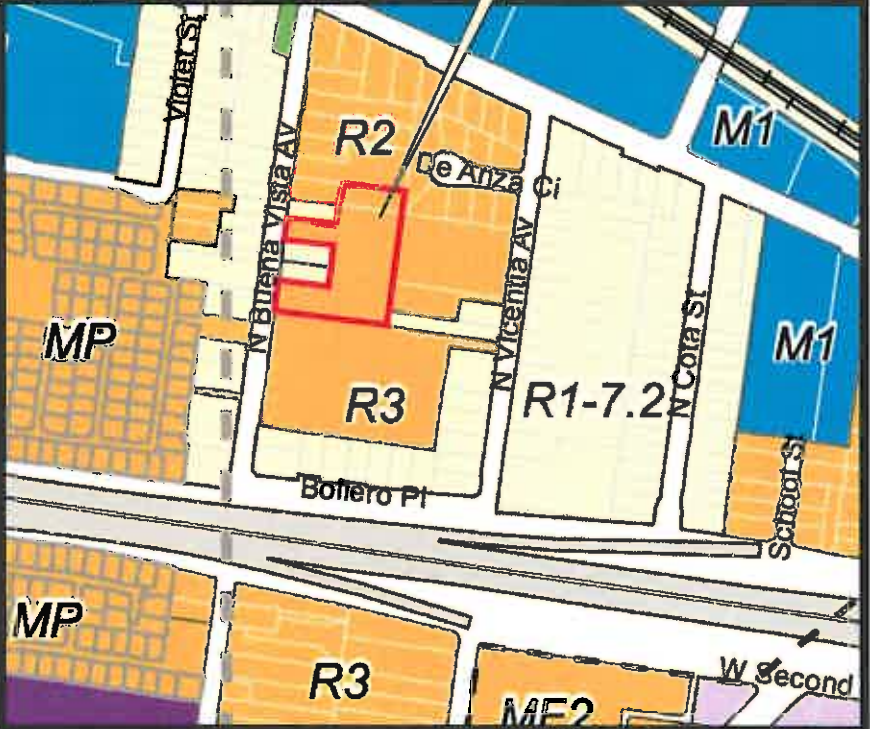
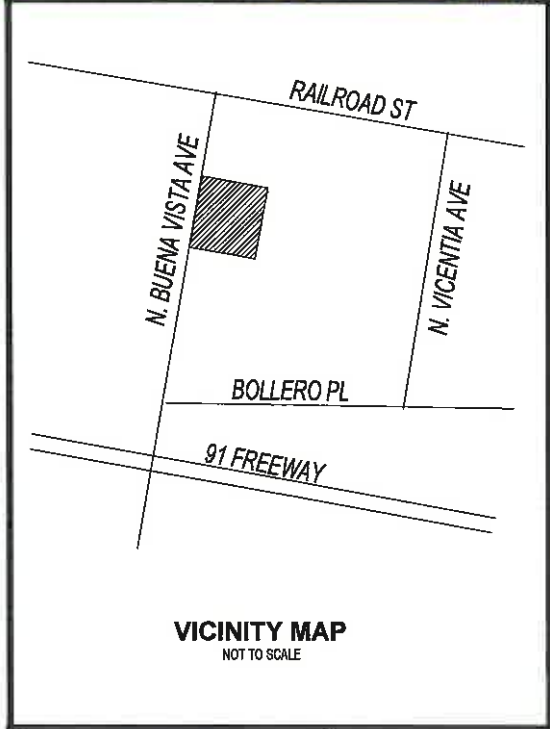
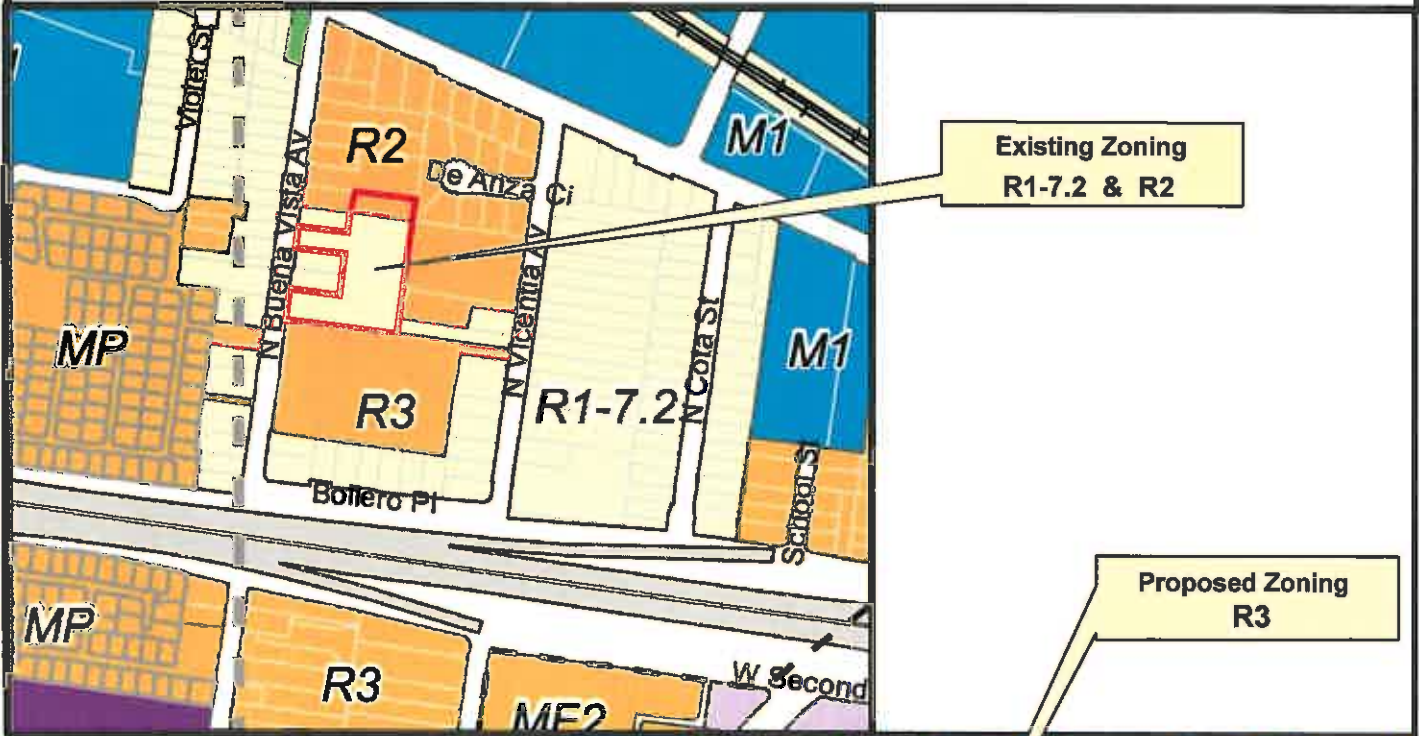
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## Notes



# Change of Zone Exhibit



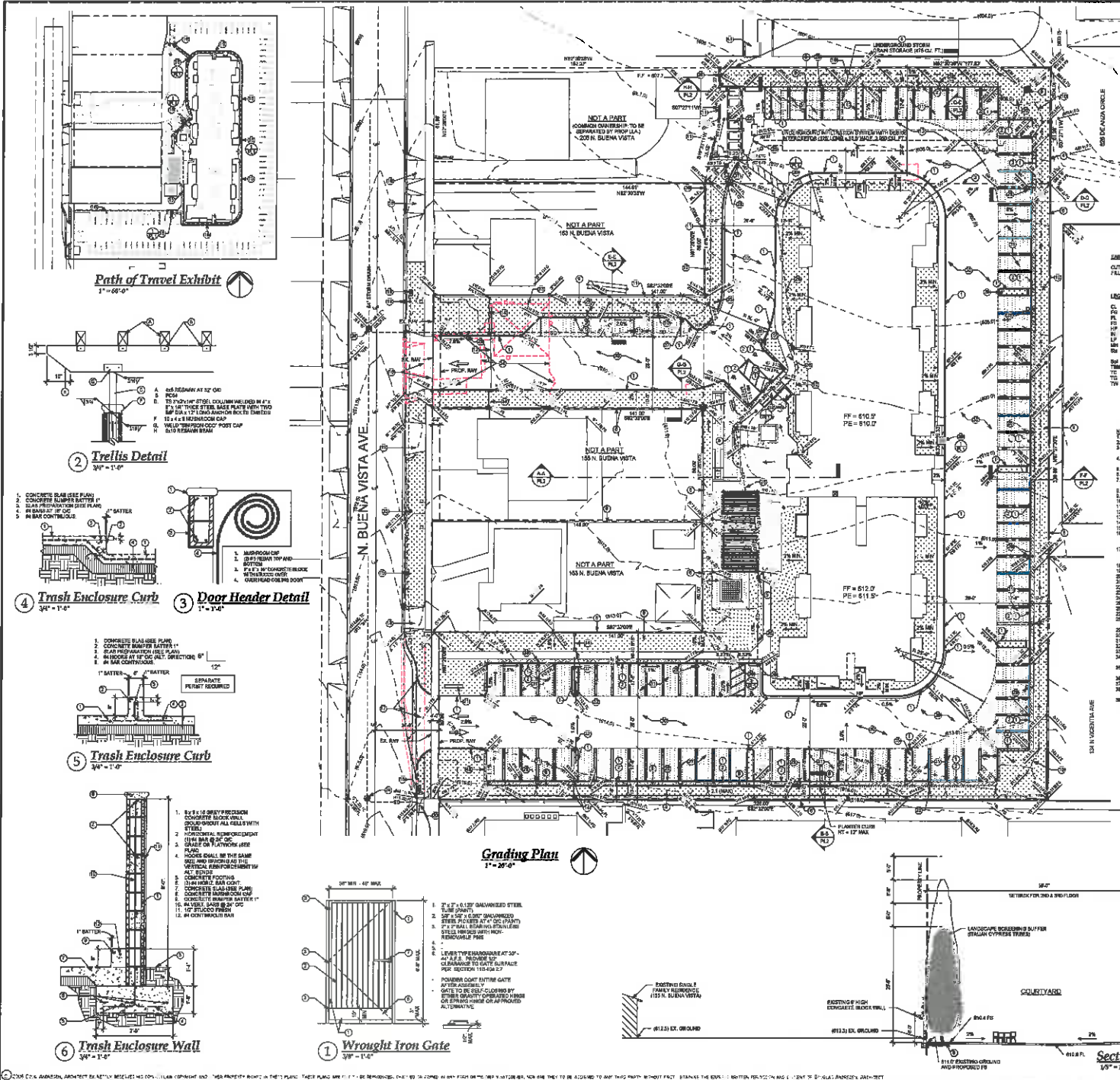
Date: 07/27/2017

CZ17-002









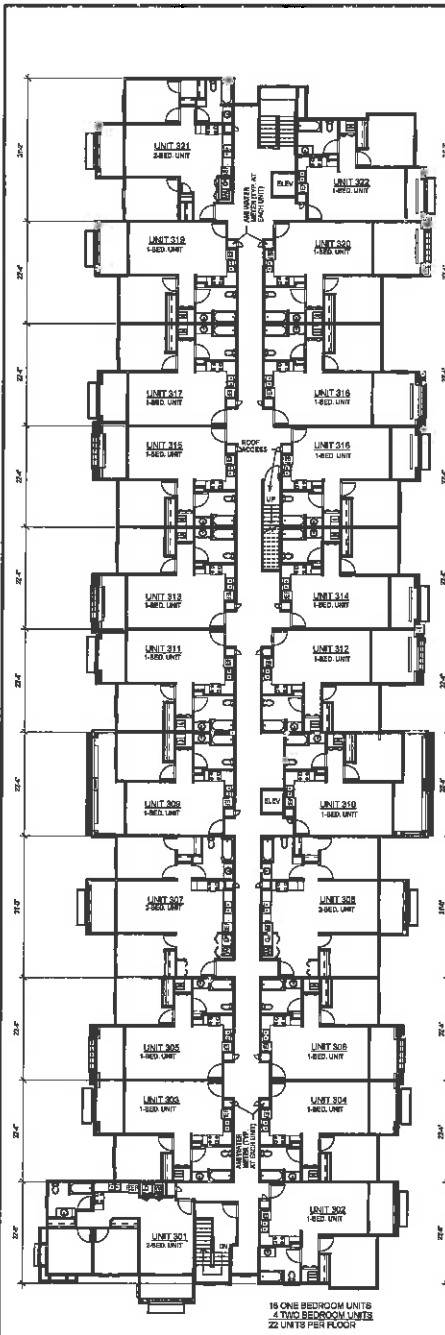
**EMBEDDING**  
 CUT/DRAW: 2-410 (3/16" YARDS AVERAGE CUT DEPTH 1.1")  
 FILL: 1,800 CUBIC YARDS AVERAGE FILL DEPTH (1.7")

**LEGEND**

- 1 CENTER LINE
- 2 FINISH GRADE
- 3 PROPOSED
- 4 EXISTING
- 5 1" = 10'
- 6 1" = 20'
- 7 1" = 40'
- 8 1" = 80'
- 9 1" = 160'
- 10 1" = 320'
- 11 1" = 640'
- 12 1" = 1280'
- 13 1" = 2560'
- 14 1" = 5120'
- 15 1" = 10240'
- 16 1" = 20480'
- 17 1" = 40960'
- 18 1" = 81920'
- 19 1" = 163840'
- 20 1" = 327680'
- 21 1" = 655360'
- 22 1" = 1310720'
- 23 1" = 2621440'
- 24 1" = 5242880'
- 25 1" = 10485760'
- 26 1" = 20971520'
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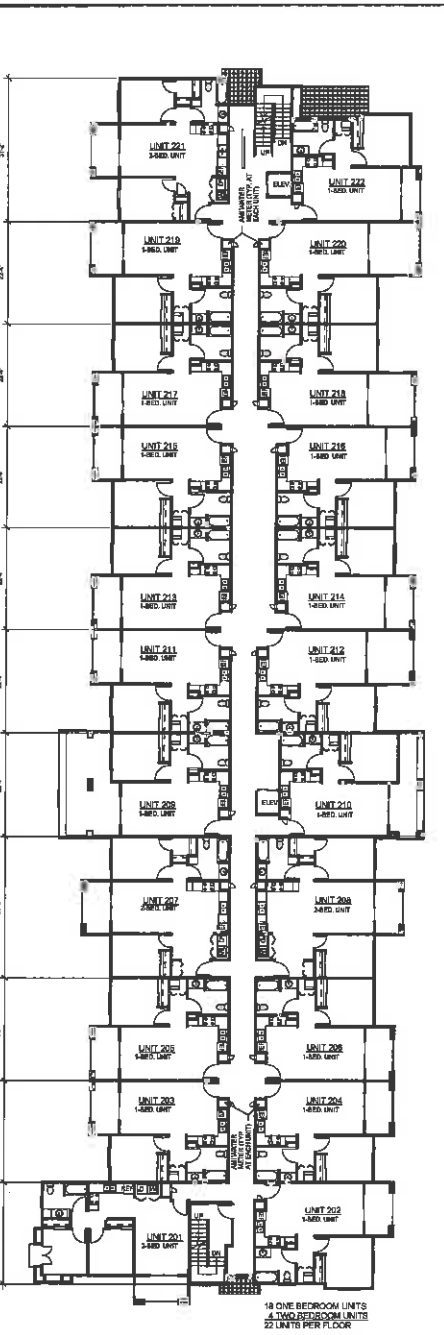






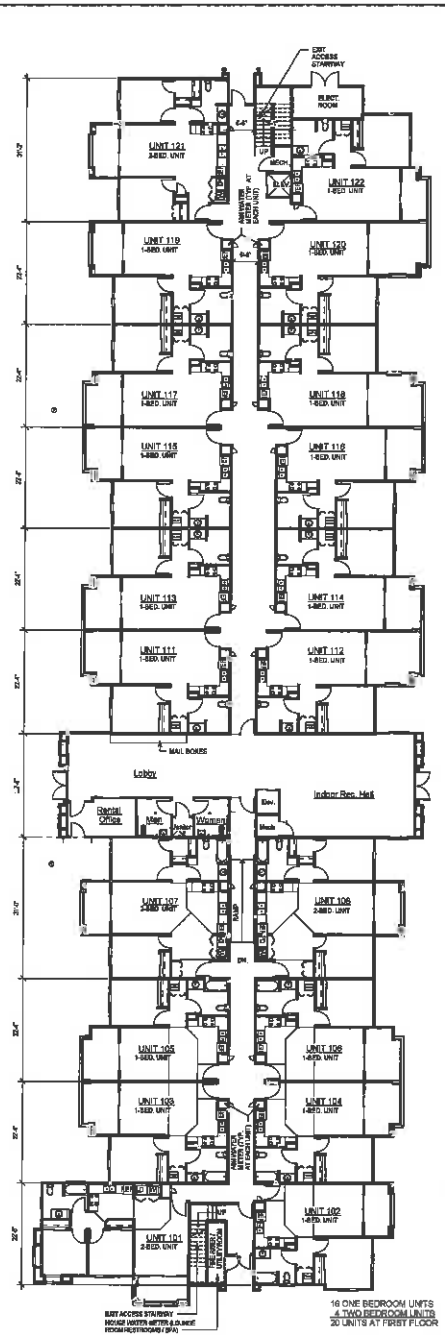
**3rd Floor Plan**  
328' x 114'

18 ONE BEDROOM UNITS  
4 TWO BEDROOM UNITS  
22 UNITS PER FLOOR



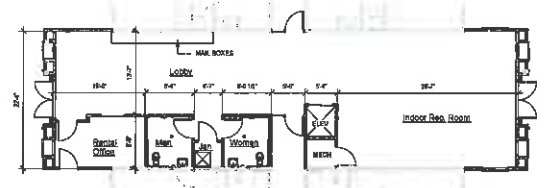
**2nd Floor Plan**  
328' x 114'

18 ONE BEDROOM UNITS  
4 TWO BEDROOM UNITS  
22 UNITS PER FLOOR

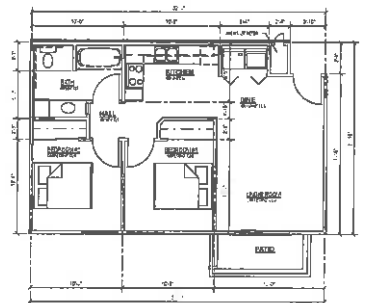


**1st Floor Plan**  
328' x 114'

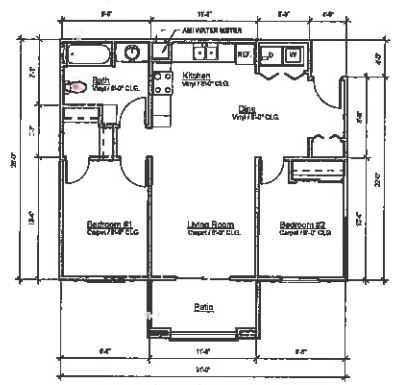
18 ONE BEDROOM UNITS  
12 TWO BEDROOM UNITS  
30 UNITS AT FIRST FLOOR



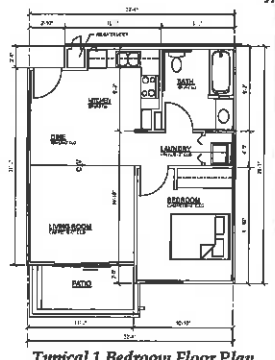
**Rec Room Floor Plan**  
168' x 114'



**Typical 2 Bedroom Floor Plan**  
318' x 114'



**Typical 2 Bedroom Floor Plan 2**  
318' x 114'



**Typical 1 Bedroom Floor Plan**  
318' x 114'

Architecture • Planning • Engineering  
**ANDRESEY**  
 Architecture  
 & INC.  
 909-355-6688  
 1700 County Road, Panama, CA 92115

Prepared under Contract No. 16-2494  
 Buena Vista Active Senior Living  
 151 N. Buena Vista, Corona, CA 92626  
 16-2494  
 16 Aug 2017

5 Aug 2016  
**Floor Plans**  
**PLA**

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# NOTICE OF PUBLIC HEARING

## RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14<sup>th</sup> Floor, Riverside, California 92501, Monday through Thursday from 8:00 a.m. to 5:00 p.m., except November 23 (Thanksgiving Day), and by prescheduled appointment on Fridays, from 9:00 a.m. to 5:00 p.m., except November 24.

ATTENTION: ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. The City of Corona will hold hearings on this project and should be contacted on non-ALUC issues.

PLACE OF HEARING: Riverside County Administration Center  
4080 Lemon St, 1<sup>st</sup> Floor Board Chambers  
Riverside, California

DATE OF HEARING: December 14, 2017

TIME OF HEARING: 9:00 A.M.

### CASE DESCRIPTION:

ZAP1008CO17 – Rowdy Williamson – City of Corona Planning Case Nos. GPA17-001 (General Plan Amendment), CZ17-002 (Change of Zone), CUP17-004 (Conditional Use Permit). The applicant proposes to establish a 64-unit, three-story senior apartment complex on 2.2 acres located at 159 and 205 Buena Vista Avenue, southerly of Railroad Street, northerly of 91 Freeway, and westerly of Vicentia Avenue. The applicant also proposes to amend the General Plan land use designation for this site from Medium Density Residential (MDR) to High Density Residential (HDR) and to change the site's zoning classification from Single Family Residential (R-1-7.2) and Low Density Multi-Family Residential (R-2) to High Density Multi-Family Residential (R-3). (Airport Compatibility Zone D of the Corona Municipal Airport Influence Area)

FURTHER INFORMATION: Contact Paul Rull at (951) 955-6893. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Ms. Terri Manuel of the City of Corona Planning Department at (951) 736-2299.



# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

GPA  
CZ  
WP

## APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP1008C017 DATE SUBMITTED: 10-30-17

### APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

Applicant Rowdy Williamson Phone Number (951) 314-2377  
 Mailing Address 2279 Eagle Glen Parkway, 112-155 Corona CA 92883 Email rowdy\_williamson@yahoo.com

Representative Rowdy Williamson Phone Number (951) 314-2377  
 Mailing Address 2279 Eagle Glen Parkway, 112-155 Corona CA 92883 Email rowdy\_williamson@yahoo.com

Property Owner Peter Nyiri Phone Number (951) 737-2121  
 Mailing Address 98 E Grand Blvd, Corona, CA 92879 Email pnryiri@earthlink.net

### LOCAL JURISDICTION AGENCY

Local Agency Name CITY OF CORONA | COMMUNITY DEVELOPMENT Phone Number 951.736.2293  
 Staff Contact Lupita Garcia, Assistant Planner Email Lupita.Garcia@CoronaCA.gov  
 Mailing Address 400 S. Vicentia Avenue, Corona, CA 92882 Case Type GPA17-001  
 Local Agency Project No GPA17-001 CZ17-002, CUP17-004

General Plan / Specific Plan Amendment  
 Zoning Ordinance Amendment  
 Subdivision Parcel Map / Tentative Tract  
 Use Permit  
 Site Plan Review/Plot Plan  
 Other

### PROJECT LOCATION

*Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways*

Street Address 159 & 205 North Buena Vista Avenue, Corona, CA 92882

Assessor's Parcel No. 118-290-025 & 118-290-030 Gross Parcel Size 2.16 Acres  
 Subdivision Name Buena Vista Active Senior Living Nearest Airport and distance from 1.55 Miles  
 Lot Number \_\_\_\_\_ Airport Corona Municipal Airport

### PROJECT DESCRIPTION

*If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed*

Existing Land Use (describe) Mostly Vacant / 1 Single Family Home on separated 8,700 SF  
Current Zone: R-1(Detached Single Family Homes)

Corona  
D

Proposed Land Use (describe)	Proposal to develop a market rate senior rental housing project consisting of 64 one and two bedroom apartment units. One 3-story building		
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)	64 Apt Units.	
For Other Land Uses (See Appendix C)	Hours of Operation		
	Number of People on Site	Maximum Number	
	Method of Calculation		
Height Data	Site Elevation (above mean sea level)	611.5	ft.
	Height of buildings or structures (from the ground)	38.3	ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?		<input type="checkbox"/> Yes
	If yes, describe		<input checked="" type="checkbox"/> No

- A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:**
1. . . . . Completed ALUC Application Form
  1. . . . . ALUC fee payment
  1. . . . . Plans Package (24x36 folded) (site plans, floor plans, building elevations, landscaping plans, grading plans, subdivision maps)
  1. . . . . Plans Package (8.5x11) (site plans, floor plans, building elevations, landscaping plans, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
  1. . . . . CD with digital files of the plans (pdf)
  1. . . . . Vicinity Map (8.5x11)
  1. . . . . Detailed project description
  1. . . . . Local jurisdiction project transmittal
  3. . . . . Gummed address labels for applicant/representative/property owner/local jurisdiction planner
  3. . . . . Gummed address labels of all surrounding property owners within a 300 foot radius of the project site (only required if the project is scheduled for a public hearing Commission meeting). If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10) with ALUC return address. \*

\* Projects involving heliports/helicopter landing sites will require additional noticing procedures.

**COUNTY OF RIVERSIDE  
AIRPORT LAND USE COMMISSION**

**STAFF REPORT**

**AGENDA ITEM:** 3.3

**HEARING DATE:** December 14, 2017

**CASE NUMBER:** ZAP1072BD17 – PV Indio, LLC (Representative: Larry Vesely)

**APPROVING JURISDICTION:** City of Indio

**JURISDICTION CASE NO:** DR 17-09-420 (Design Review)

**MAJOR ISSUES:** None

**RECOMMENDATION:** Staff recommends that the Commission find the Design Review CONSISTENT, subject to the conditions included herein.

**PROJECT DESCRIPTION:** A proposal to install canopies over most of the recreational vehicle (RV)/boat storage parking spaces at an existing 4.5 acre self-storage/RV/boat storage facility. The applicant also proposes installing a 110.76 kW photovoltaic solar panel system on the roof of two proposed canopies.

**PROJECT LOCATION:** The site is located at 42925 Madison Street, westerly of Madison Street, southerly of Indio Boulevard and the southerly terminus of Madio Street, and northerly of Fox Glove Lane, in the City of Indio, approximately 4,800 feet southeasterly of runway 10-28 at Bermuda Dunes Airport.

**LAND USE PLAN:** 2004 Bermuda Dunes Airport Land Use Compatibility Plan

- a. Airport Influence Area: Bermuda Dunes Airport
- b. Land Use Policy: Compatibility Zones B1, C
- c. Noise Levels: 55 – 60 CNEL contour

**BACKGROUND:**

Flight Hazard Issues: Structure height, electrical interference, and reflectivity/glare are among the issues that solar panels in the airport influence area must address. The project's 110.76kW photovoltaic (PV) system would be located on the top of two of the proposed canopies within



## Compatibility Zones B1 and C.

### *Electrical and Communication Interference*

The applicant has indicated that they do not plan to utilize equipment that would interfere with aircraft communications. The PV panels themselves present little risk of interfering with radar transmission due to their low profiles. In addition, solar panels do not emit electromagnetic waves over distances that could interfere with radar signal transmissions, and any electrical facilities that do carry concentrated current will be buried beneath the ground and away from any signal transmission. There is no radar transmission or receiving facilities within the site.

### *Glint and Glare/Reflectivity*

The applicant has prepared a glare analysis utilizing the web based Solar Glare Hazard Analysis Tool (SGHAT) and provided a report, a copy of which is attached hereto. The analysis was based on straight in approach patterns to both runways 10 and 28. The analysis utilized a glide slope approach of 3.0 degrees for the approach, a 10 degree tilt, and array orientation of 180 degrees.

The solar glare analysis concluded that no glare would occur on the approach to runways 10 and 28, largely due to the following reasons:

- the maximum 14 foot tall and 6,760 square feet area of the canopy/solar panels are not of a significant size to create glare that would impact pilots on the flight path,
- the solar panels are located at a significant distance away from the runway and flightpath; approximately 4,800 feet easterly and 1,100 feet southerly of the straight in approach to the easterly end of the runway, and
- the flight path and aircraft on approach to the runway is located north of the solar panels, and the panels are orientated facing south and away from the flight path.

Non-Residential Intensity: Pursuant to the 2004 Bermuda Dunes Airport Land Use Compatibility Plan, the project site is located within Compatibility Zones B1 and C. Compatibility Zone B1 restricts average intensity to 25 people per acre and 50 people per single acre. Compatibility Zone C restricts average intensity to 75 people per acre and 150 people per single acre.

The project proposes eight metal canopies (ranging from 5,974 to 6,000 square feet) totaling 47,918 square feet over the existing RV/boat storage parking area. The canopies will be located in both Compatibility Zones B1 and C. The freestanding open air canopies are proposed to provide shade to the vehicles. Two of the proposed canopies totaling 6,760 square feet (B1 and B2) will have a 110.76 kW photovoltaic solar panel system installed on the canopy roof. The project will not generate any new occupancy/intensity.

Prohibited and Discouraged Uses: The applicant does not propose any uses specifically prohibited or discouraged in Compatibility Zones B1 or C of the Bermuda Dunes Airport Influence Area.

Noise: The site is located within the 55 - 60 CNEL contour range from aircraft noise. The project does not generate any new occupancy that would be subject to noise impacts. The existing commercial use is not sensitive to noise, and would not require special measures to mitigate aircraft-generated noise.

Part 77: The elevation of Runway 10-28 at its easterly terminus is approximately 49 feet above mean sea level (AMSL). At a distance of approximately 4,800 feet from the runway, FAA review would be required for any structures with top of roof exceeding 97 feet AMSL. The project's site elevation is 23 feet AMSL, and the height of the canopy is 14 feet, for a maximum top point elevation of 37 feet AMSL. Therefore, Federal Aviation Administration (FAA) obstruction evaluation review for height/elevation reasons is not required.

Open Area: The site is located within Airport Compatibility Zones B1 and C. Compatibility Zone B1 requires projects 10 acres or larger to designate 30% of project area as ALUC-qualifying open area that could potentially serve as emergency landing areas, and Compatibility Zone C requires 20%. Based on the project's size of 4.5 acres, the provision of open area is not required.

#### **CONDITIONS:**

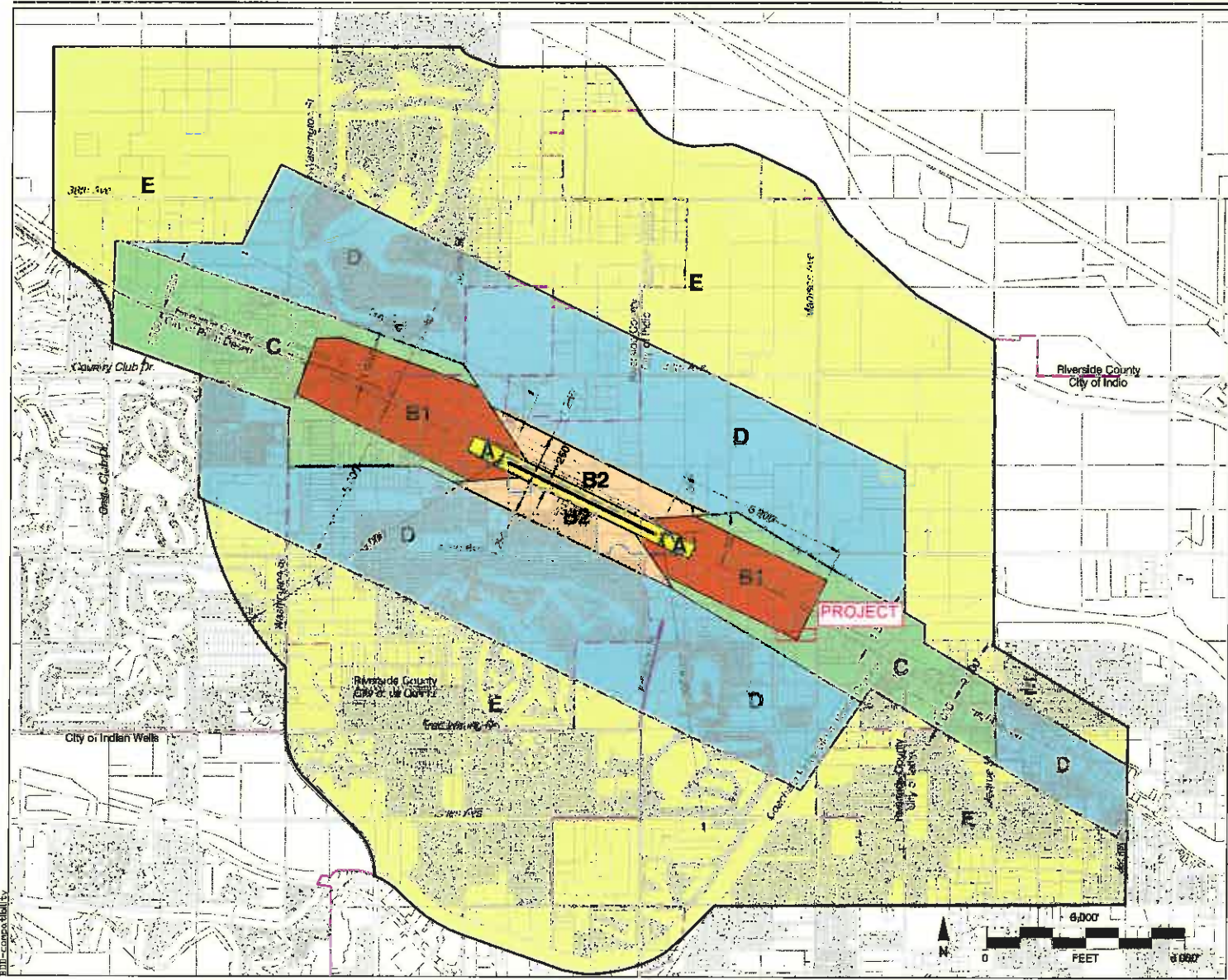
1. Any outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. All outdoor lighting plans shall be subject to review by airport management.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
  - (a) Any use or activity which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use or activity which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use or activity which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, composting operations, production of cereal grains, sunflower, and row crops, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash

disposal, and incinerators.)

- (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Children's schools, day care centers, libraries, hospitals, nursing homes, highly noise-sensitive outdoor nonresidential uses, places of worship, aboveground bulk storage of 6,000 gallons or more of flammable or hazardous materials, and hazards to flight.
3. The attached notice shall be given to all prospective purchasers of the property.
  4. Prior to issuance of a building permit, the property owner shall convey an avigation easement to Bermuda Dunes Airport or provide documentation that such easement was previously conveyed. Copies of the recorded avigation easement shall be forwarded to the Airport Land Use Commission and to the City of Indio.
  5. Any new detention basin(s) on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
  6. Any revisions to the solar panels will require a new solar glare analysis to ensure that the project does not create significant amounts of glare, and require ALUC review.

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



**Legend**

- Compatibility Zones**
- Airport Influence Area Boundary
  - Zone A
  - Zone B1
  - Zone B2
  - Zone C
  - Zone D
  - Zone E

- Boundary Lines**
- Airport Property Line
  - City Limits

**Note**  
 Southwestern edge of Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

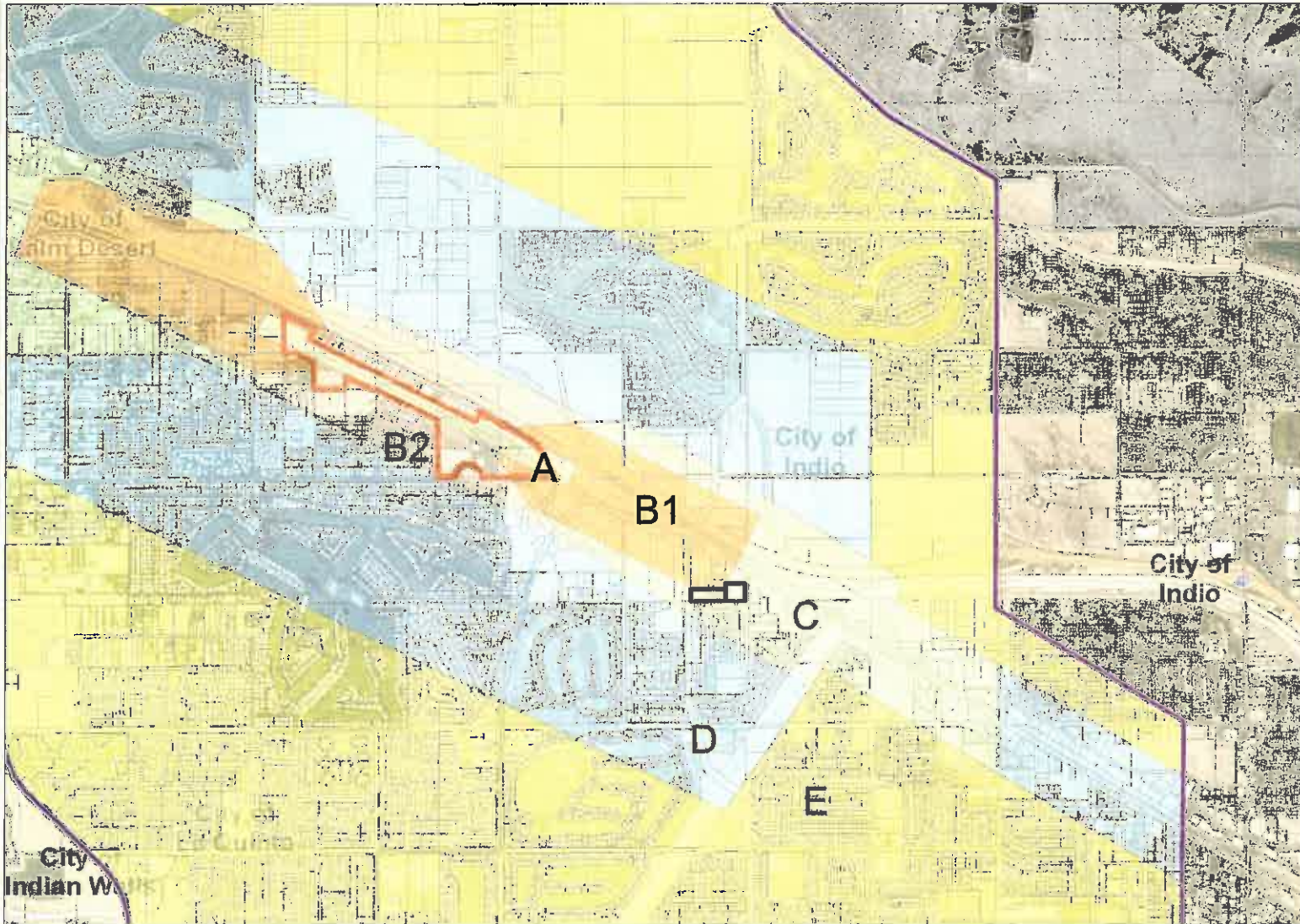
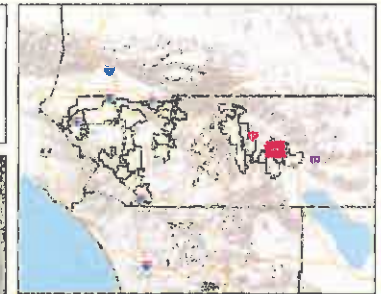
See Chapter 2, Table 2A for compatibility criteria associated with this map.

**Riverside County**  
**Airport Land Use Commission**  
**Riverside County**  
**Airport Land Use Compatibility Plan**  
**Policy Document**  
 (Adopted December 2004)

Map BD-1

**Compatibility Map**  
Bermuda Dunes Airport

# My Map



## Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



0 4,004 8,009 Feet



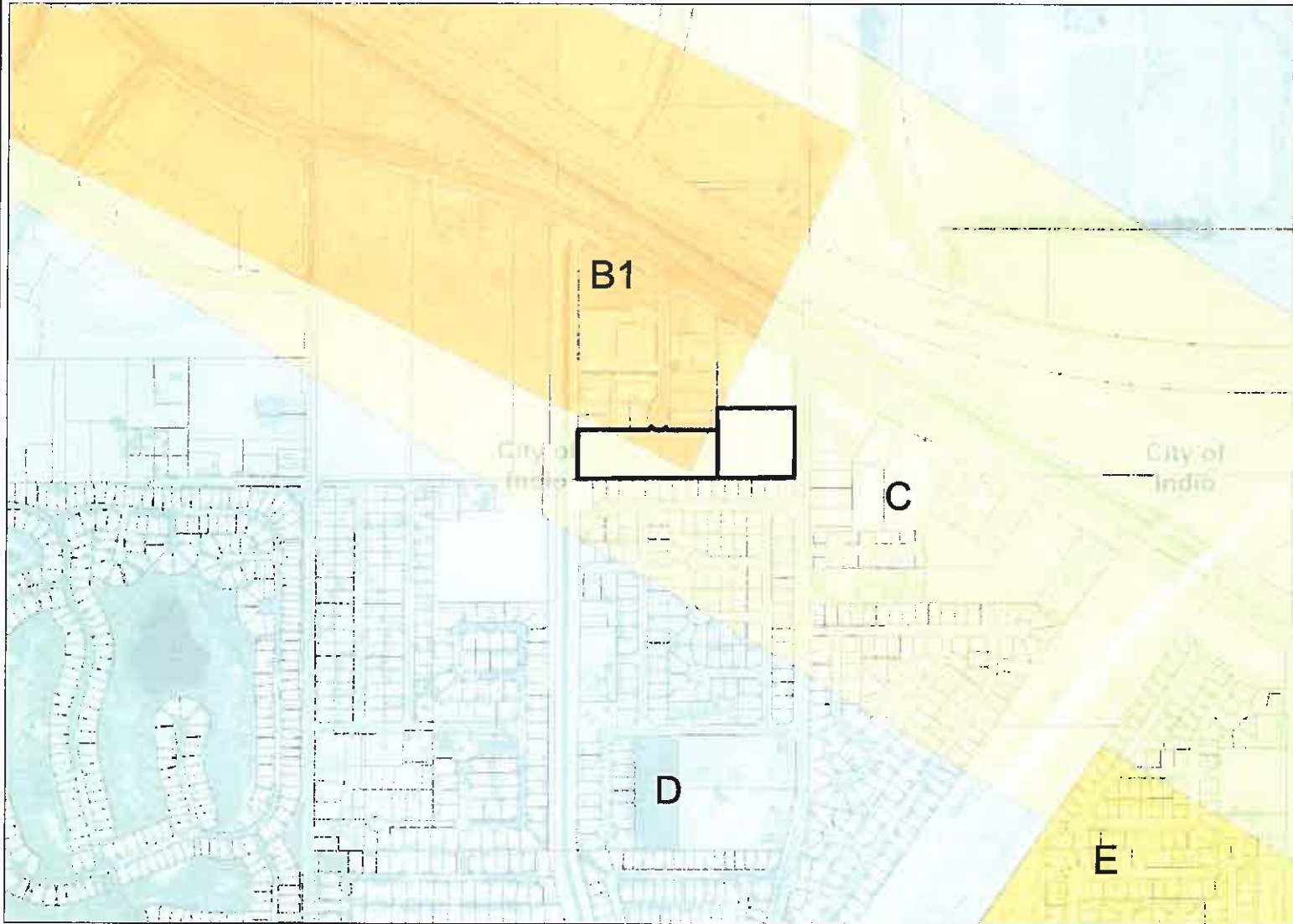
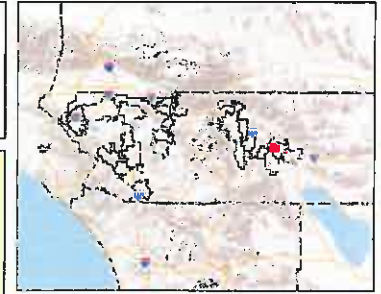
**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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## Notes

# My Map



## Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



0 1,001 2,002 Feet



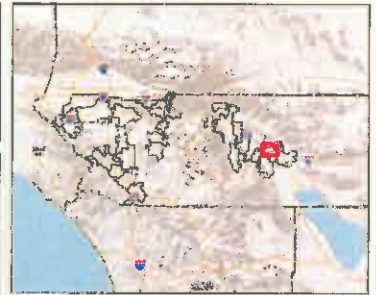
**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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## Notes

# My Map



## Legend

- City Boundaries
- Cities**
- roads**
- sanno**
- highways**
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads**
- Major Roads
- Arterial
- Collector
- Residential
- counties**
- cities**
- hydrography**
- lines**
- waterbodies**
- Lakes
- Rivers



0      4,004      8,009 Feet



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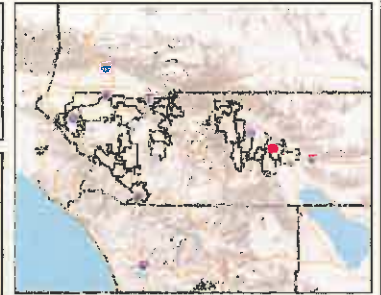
REPORT PRINTED ON... 11/1/2017 11:04:03 AM

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## Notes



# My Map



## Legend

-  City Boundaries
-  Cities
-  roads
-  highways
-  HWY
-  INTERCHANGE
-  INTERSTATE
-  OFFRAMP
-  ONRAMP
-  USHWY
-  counties
-  cities
-  hydrography
-  lines
-  waterbodies
-  Lakes
-  Rivers



0 1,001 2,002 Feet



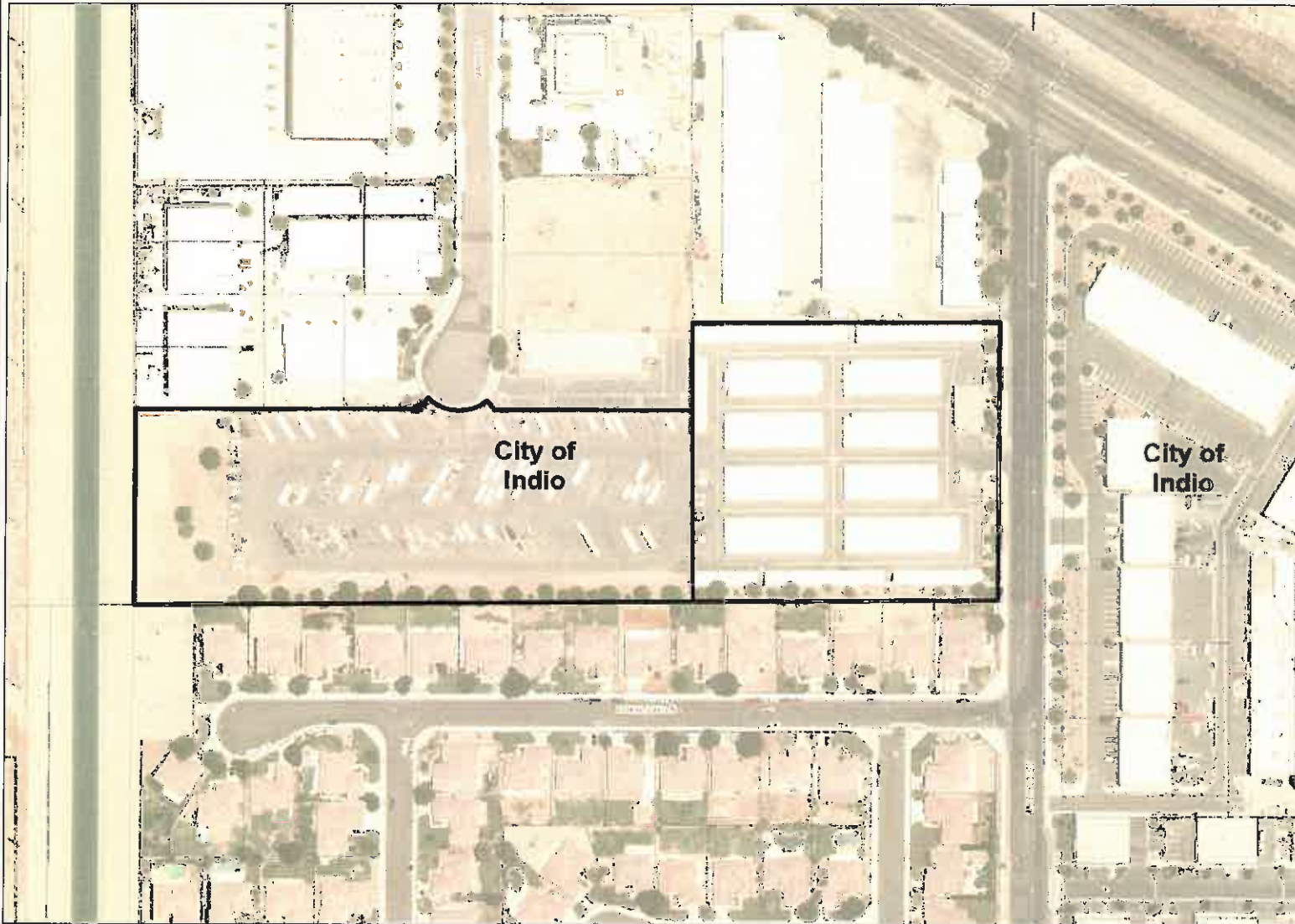
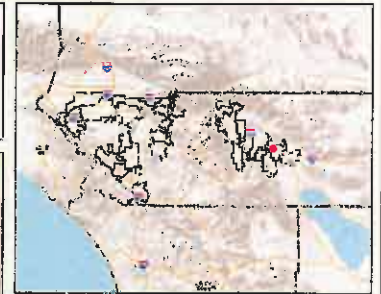
**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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## Notes

# My Map



### Legend

- City Boundaries
- Cities**
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 250 501 Feet



**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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### Notes

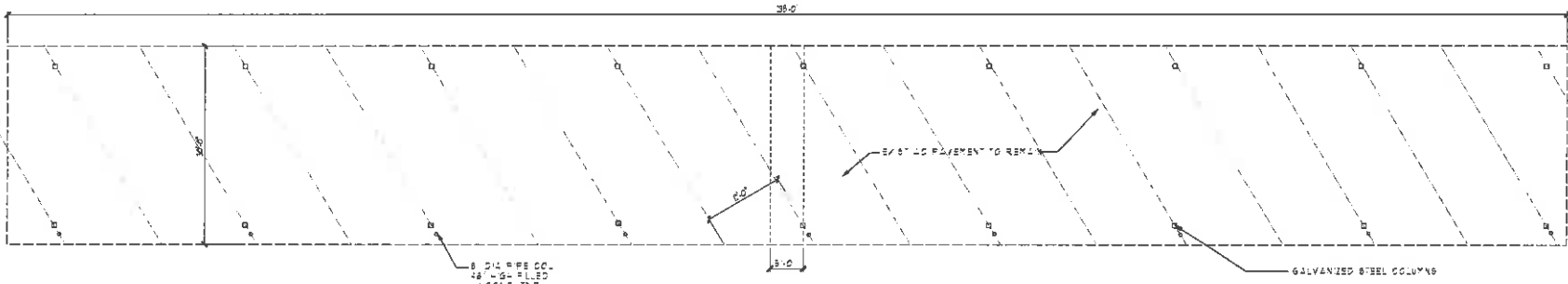




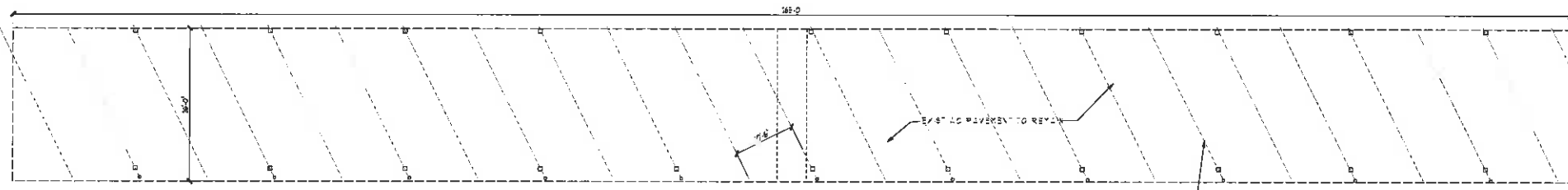



JOB NO. 9-03  
 DATE 03-26-11  
 SCALE 1/8" = 1'-0"  
 SHEET  
**2**

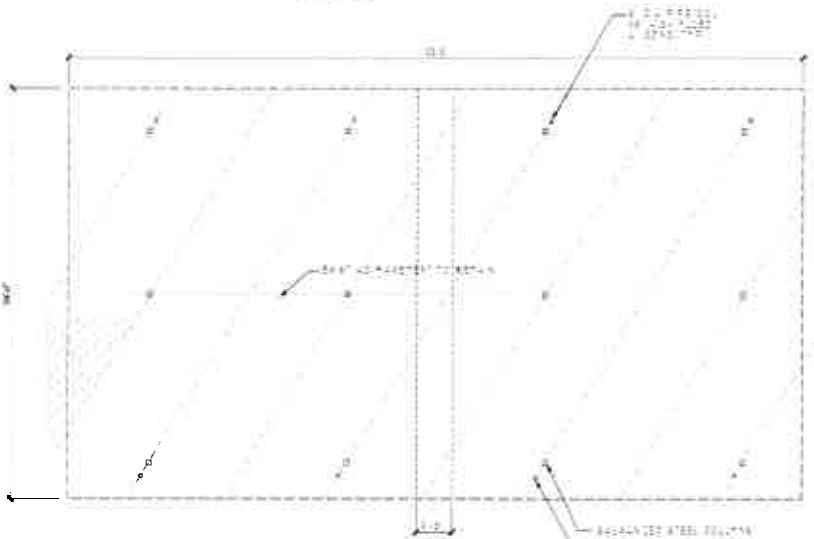
FLOOR PLAN



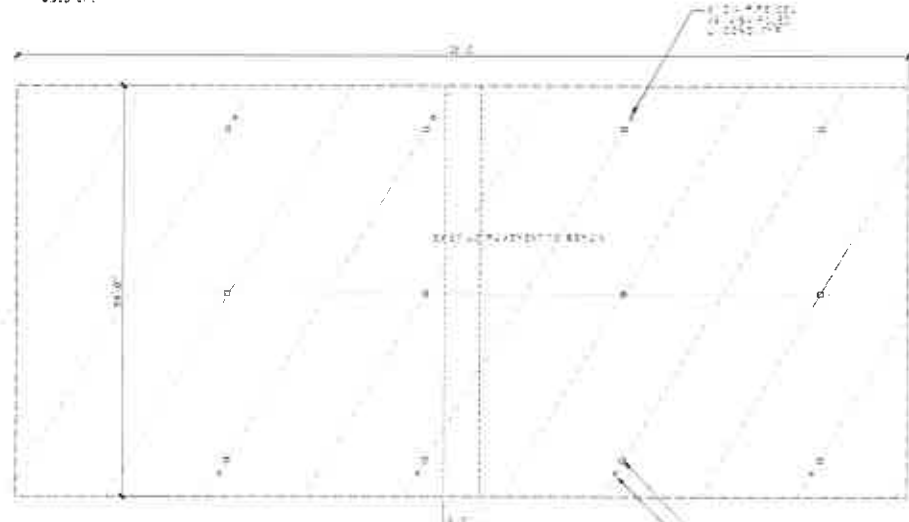
**CANOPY A PLAN**  
6,000 S.F.



**CANOPY B PLAN**  
6,980 S.F.

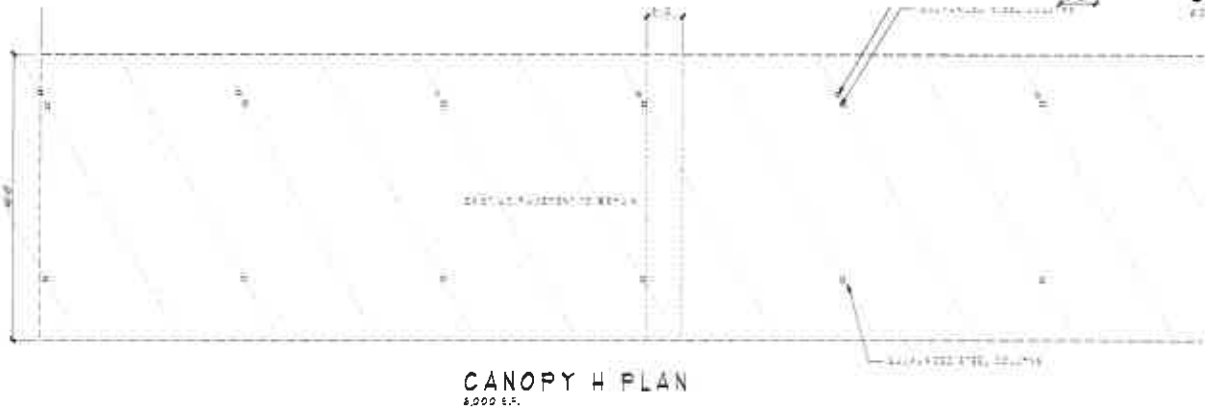
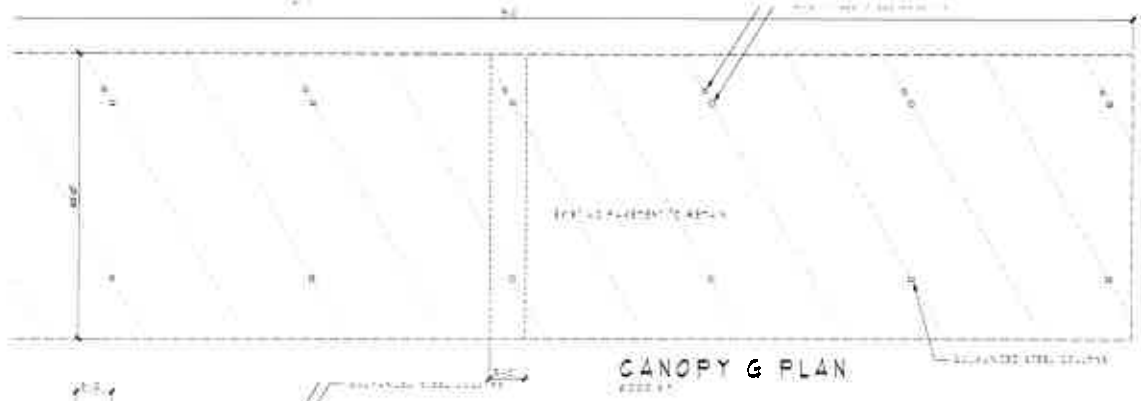
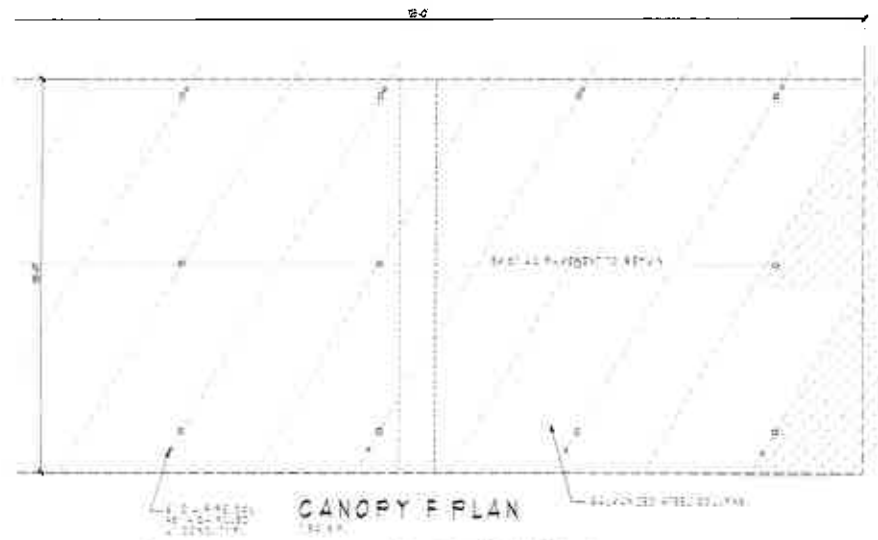
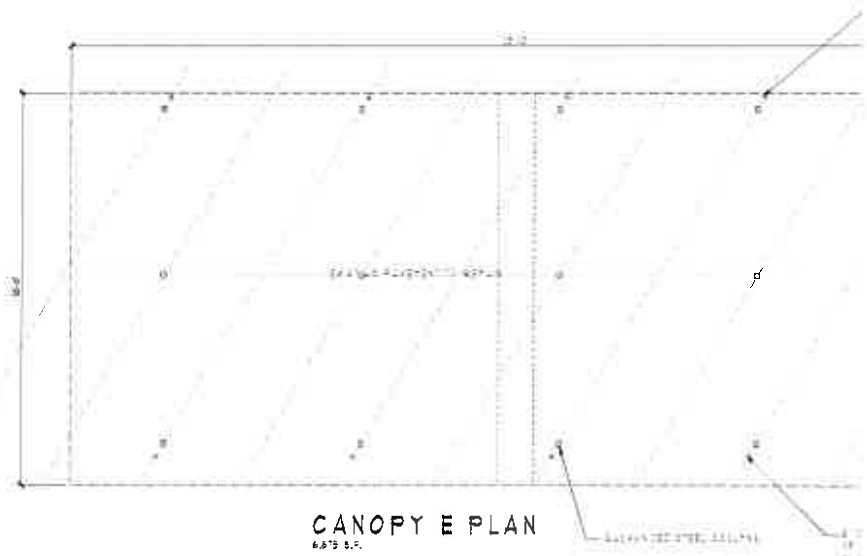


**CANOPY C PLAN**  
5,974 S.F.



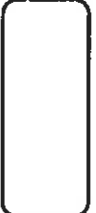
**CANOPY D PLAN**  
7,250 S.F.

C:\DRAWING\90252-01\90252-01-02\FLOOR PLAN.dwg



1. FINISH FLOOR: 4" REINFORCED CONCRETE  
 2. FINISH FLOOR: 4" REINFORCED CONCRETE

**VESELY ARCHITECT**  
**PLANNING**  
 ARCHITECTURE  
 1420 West Park  
 Suite 100  
 San Jose, CA 95128  
 408.734.8800  
 www.veselyarchitect.com



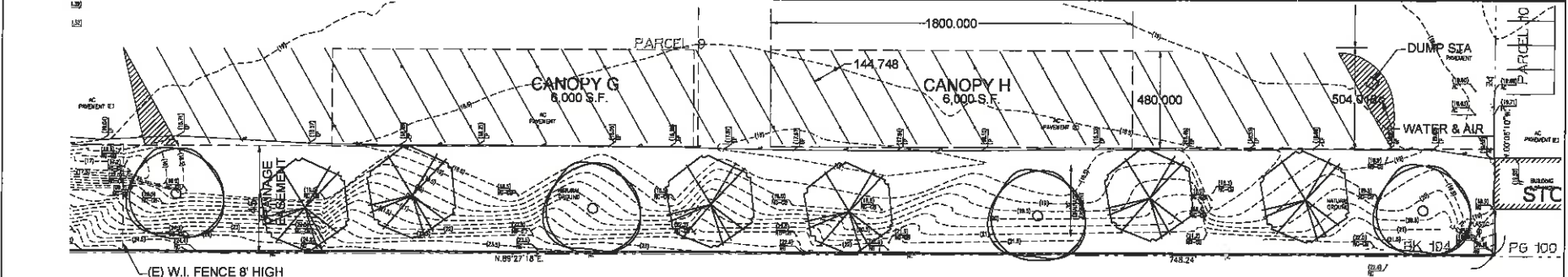
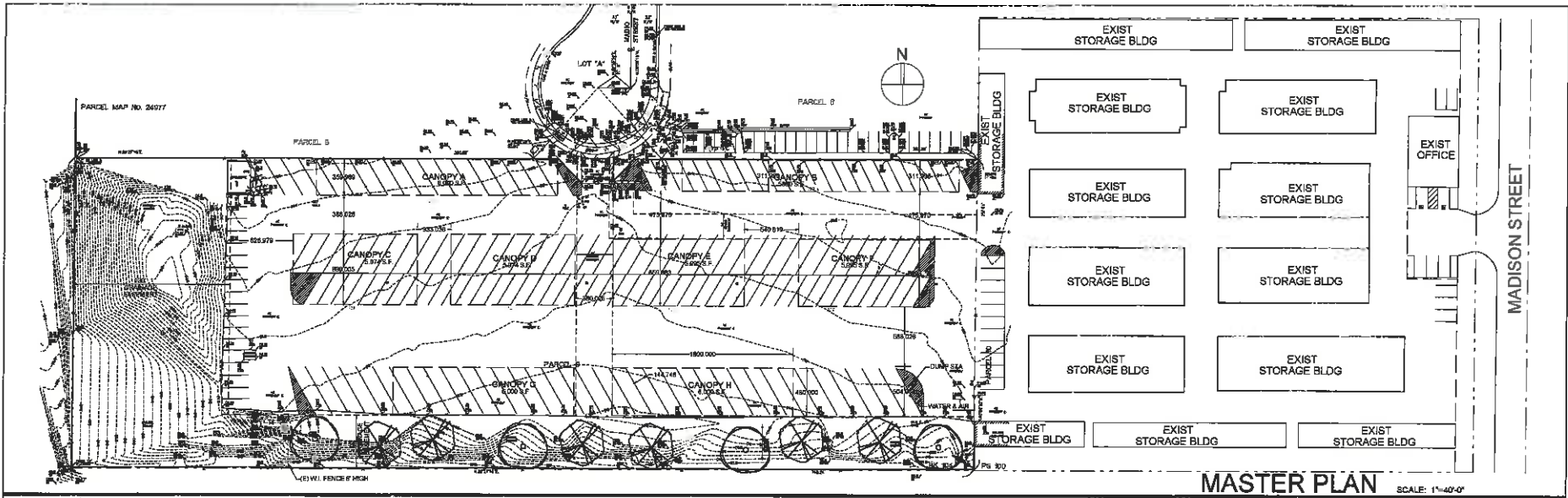
RV CANOPIES FOR  
**DESERT STORAGE MASTERS**  
 42825 MADISON ST.  
 INDIO, CA 92201




JOB NO. 2-23  
 DATE OF DRAWING  
 SCALE 1/8" = 1'-0"  
 SHEET  
**2.1**

FLOOR PLANS





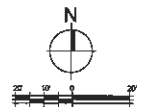
CERCIDIUM 'DESERT MUSEUM'



PROSOPIS CHILENSIS THORNLESS

### DETAILED PLANTING PLAN

SCALE: 1"=20'-0"



#### PLANTING LEGEND

PLANTING SIZE	SCIENTIFIC NAME	COMMON NAME	WATER NEEDS	PLANT FACTOR
4 30" BOX Multi Tr.	PROSOPIS CHILENSIS	CHIL BAY MESQUITE THORNLESS	LOW	0.2
8 30" BOX Multi Tr.	CERCIDIUM 'DESERT MUSEUM'	'PALO VERDE	LOW	0.2

25 TONS 2" DEPTH 3/8" OR 3/4" GRAY GRAVEL BY WHITEWATER ROCK AND SUPPLY

#### PLANTING NOTES

- PLANTING AREAS TO RECEIVE A 2" MIN. LAYER OF 3/8" OR 3/4" GRAY GRAVEL BY WHITEWATER ROCK AND SUPPLY
- PLANT QUANTITIES INDICATED IN THE PLANT LIST ARE FOR CONTRACTOR'S CONVENIENCE ONLY. CONTRACTOR TO VERIFY PLANT QUANTITIES PER PLAN PRIOR TO BID. CONTRACTOR TO PROVIDE AND INSTALL ALL PLANTS INDICATED PER PLAN.
- ALL TREES WITHIN FIVE FEET OF PAVING SHALL BE INSTALLED WITH APPROVED DEEP ROOT BARRIER.

LANDSCAPE ARCHITECTURAL SERVICES BY:  
**DESERT MODERN**  
LANDSCAPE DESIGN

LANDSCAPE PLANS FOR:  
RV CANOPIES FOR  
**DESERT STORAGE MASTERS**  
42825 MADISON STREET  
INDIO, CALIFORNIA

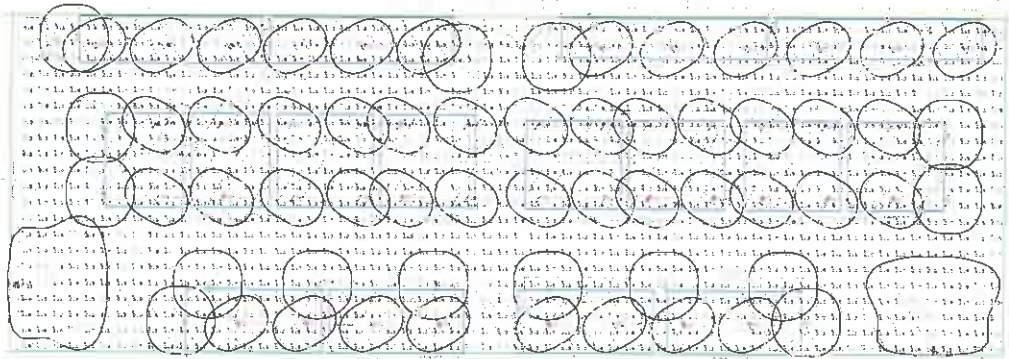
DESERT MODERN  
LANDSCAPE DESIGN

REVISION
08-12-17
08-28-17

SCALE: 1"=20'-0"

SHEET  
**L-1.0**





1. 1974-1975, 1976-1977  
 2. 1978-1979, 1980-1981  
 3. 1982-1983, 1984-1985  
 4. 1986-1987, 1988-1989  
 5. 1990-1991, 1992-1993  
 6. 1994-1995, 1996-1997  
 7. 1998-1999, 2000-2001  
 8. 2002-2003, 2004-2005  
 9. 2006-2007, 2008-2009  
 10. 2010-2011, 2012-2013  
 11. 2014-2015, 2016-2017  
 12. 2018-2019, 2020-2021

Year	Value	Category
1974-1975	100	High
1976-1977	100	High
1978-1979	100	High
1980-1981	100	High
1982-1983	100	High
1984-1985	100	High
1986-1987	100	High
1988-1989	100	High
1990-1991	100	High
1992-1993	100	High
1994-1995	100	High
1996-1997	100	High
1998-1999	100	High
2000-2001	100	High
2002-2003	100	High
2004-2005	100	High
2006-2007	100	High
2008-2009	100	High
2010-2011	100	High
2012-2013	100	High
2014-2015	100	High
2016-2017	100	High
2018-2019	100	High
2020-2021	100	High

1. 1974-1975, 1976-1977  
 2. 1978-1979, 1980-1981  
 3. 1982-1983, 1984-1985  
 4. 1986-1987, 1988-1989  
 5. 1990-1991, 1992-1993  
 6. 1994-1995, 1996-1997  
 7. 1998-1999, 2000-2001  
 8. 2002-2003, 2004-2005  
 9. 2006-2007, 2008-2009  
 10. 2010-2011, 2012-2013  
 11. 2014-2015, 2016-2017  
 12. 2018-2019, 2020-2021



DESERT STORAGE  
42925 MADISON STREET  
INDIO, CA 92201

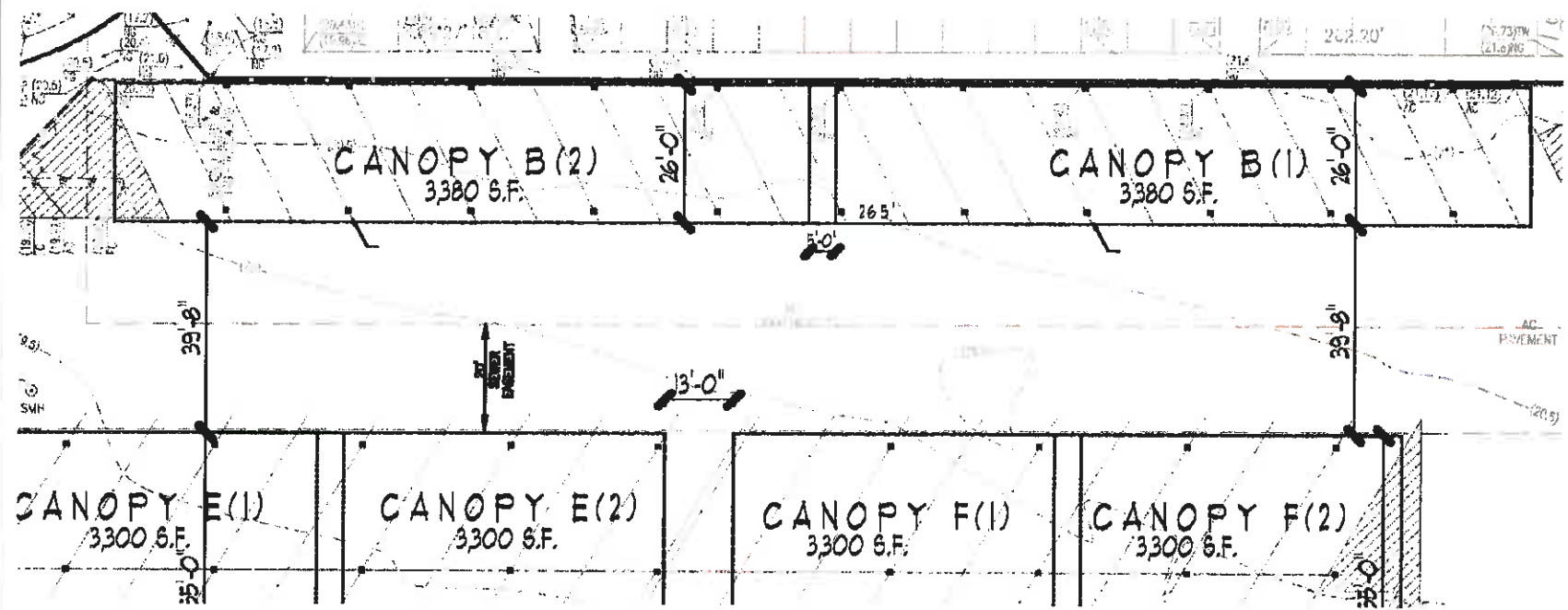
2913 TECH CENTER DR  
SANTA ANA, CA 92705  
949-229-5977

**ECOFORCE**  
SOLUTIONS



CANOPY B PLAN  
4310 EA

(312) HYUNDAI 355'S - 110.76kW (DC-STC)





# FORGESOLAR GLARE ANALYSIS

Project: **42925 Madison St - RV Canopies**

Site configuration: **ZAP1072BD17 Madison Street RV canopies**

Analysis conducted by William Diehl (wil@ecoforce-solutions.com) at 01:17 on 08 Nov, 2017.

## U.S. FAA 2013 Policy Adherence

The following table summarizes the policy adherence of the glare analysis based on the 2013 U.S. Federal Aviation Administration Interim Policy 78 FR 63276. This policy requires the following criteria be met for solar energy systems on airport property:

- No "yellow" glare (potential for after-image) for any flight path from threshold to 2 miles
- No glare of any kind for Air Traffic Control Tower(s) ("ATCT") at cab height.
- Default analysis and observer characteristics (see list below)

ForgeSolar does not represent or speak officially for the FAA and cannot approve or deny projects. Results are informational only.

COMPONENT	STATUS	DESCRIPTION
Analysis parameters	PASS	Analysis time interval and eye characteristics used are acceptable
Flight path(s)	PASS	Flight path receptor(s) do not receive yellow glare
ATCT(s)	N/A	No ATCT receptors designated

Default glare analysis and observer eye characteristics are as follows:

- Analysis time interval: 1 minute
- Ocular transmission coefficient: 0.5
- Pupil diameter: 0.002 meters
- Eye focal length: 0.017 meters
- Sun subtended angle: 9.3 milliradians

FAA Policy 78 FR 63276 can be read at <https://www.federalregister.gov/d/2013-24729>

# SITE CONFIGURATION

## Analysis Parameters

DN: peaks at 1,000.0 W/m<sup>2</sup>  
Time Interval: 1 min  
Ocular transmission coefficient: 0.5  
Pupil diameter: 0.002 m  
Eye focal length: 0.017 m  
Sun subtended angle: 9.3 mrad  
Site Config ID: 11137.1960



## PV Array(s)

**Name:** RV Canopies  
**Axis tracking:** Fixed (no rotation)  
**Tilt:** 10.0°  
**Orientation:** 180.0°  
**Rated power:** 110.76 kW  
**Panel material:** Smooth glass without AR coating  
**Reflectivity:** Vary with sun  
**Slope error:** 6.55 mrad



Vertex	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
1	33.737313	-116.253939	19.91	14.00	33.91
2	33.737313	-116.253017	25.76	14.00	39.76
3	33.737240	-116.253017	25.14	14.00	39.14
4	33.737235	-116.253945	21.66	14.00	35.66

## Flight Path Receptor(s)

**Name:** Approach from NW

**Description:**

**Threshold height:** 50 ft

**Direction:** 116.38°

**Glide slope:** 3.0°

**Pilot view restricted?** Yes

**Vertical view:** 30.0°

**Azimuthal view:** 120.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.754602	-116.290369	93.07	50.00	143.07
Two-mile	33.741756	-116.259179	32.35	664.17	696.52

**Name:** Approach from SE

**Description:**

**Threshold height:** 50 ft

**Direction:** 297.18°

**Glide slope:** 3.0°

**Pilot view restricted?** Yes

**Vertical view:** 30.0°

**Azimuthal view:** 120.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.736118	-116.246252	17.36	50.00	67.37
Two-mile	33.749325	-116.277216	63.03	557.79	620.82

## GLARE ANALYSIS RESULTS

### Summary of Glare

PV Array Name	Tilt (°)	Orient (°)	"Green" Glare min	"Yellow" Glare min	Energy kWh
RV Canopies	10.0	180.0	0	0	247,600.0

*Total annual glare received by each receptor*

<b>Receptor</b>	<b>Annual Green Glare (min)</b>	<b>Annual Yellow Glare (min)</b>
Approach from NW	0	0
Approach from SE	0	0

## **Results for: RV Canopies**

<b>Receptor</b>	<b>Green Glare (min)</b>	<b>Yellow Glare (min)</b>
Approach from NW	0	0
Approach from SE	0	0

### **Flight Path: Approach from NW**

0 minutes of yellow glare  
0 minutes of green glare

### **Flight Path: Approach from SE**

0 minutes of yellow glare  
0 minutes of green glare

## **Assumptions**

"Green" glare is glare with low potential to cause an after-image (flash blindness) when observed prior to a typical blink response time.

"Yellow" glare is glare with potential to cause an after-image (flash blindness) when observed prior to a typical blink response time.

Times associated with glare are denoted in Standard time. For Daylight Savings, add one hour.

Glare analyses do not account for physical obstructions between reflectors and receptors. This includes buildings, tree cover and geographic obstructions.

The glare hazard determination relies on several approximations including observer eye characteristics, angle of view, and typical blink response time. Actual values may differ.

Hazard zone boundaries shown in the Glare Hazard plot are an approximation and visual aid based on aggregated research data. Actual ocular impact outcomes encompass a continuous, not discrete, spectrum.

# NOTICE OF PUBLIC HEARING

## RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14<sup>th</sup> Floor, Riverside, California 92501, Monday through Thursday from 8:00 a.m. to 5:00 p.m., except November 23 (Thanksgiving Day), and by prescheduled appointment on Fridays, from 9:00 a.m. to 5:00 p.m., except November 24.

ATTENTION: ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. The City of Indio may hold hearings on this project and should be contacted on non-ALUC issues.

PLACE OF HEARING: Riverside County Administration Center  
4080 Lemon St, 1<sup>st</sup> Floor Board Chambers  
Riverside, California

DATE OF HEARING: December 14, 2017

TIME OF HEARING: 9:00 A.M.

### CASE DESCRIPTION:

ZAP1072BD17 – PV Indio, LLC (Representative: Larry Vesely) – City of Indio Planning Case No. DR 17-09-420 (Design Review). The applicant proposes installing canopies over most of the recreational vehicle (RV)/boat storage parking spaces at an existing 4.5 acre self-storage/RV/boat storage facility located at 42925 Madison Street, westerly of Madison Street, southerly of Indio Boulevard and the southerly terminus of Madio Street, and northerly of Fox Glove Lane. The applicant also proposes installing a photovoltaic solar panel system on the roof of two of the proposed canopies. (Airport Compatibility Zones B1 and C of the Bermuda Dunes Airport Influence Area)

FURTHER INFORMATION: Contact Paul Rull at (951) 955-6893. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Ms. Rosie Lua of the City of Indio Planning Department at (760) 391-4016.



# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

## APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP 1072BD 11 DATE SUBMITTED: 10-31-17

### APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

Applicant	PV Indio, LLC	Phone Number	858-625-0100
Mailing Address	4350 La Jolla Village Dr. #110 San Diego, ca 92122	Email	kwechter@pacventures.com

Representative	Larry Vesely, Architect	Phone Number	951-784-2570
Mailing Address	1420 Basel Pl Riverside, CA 92506	Email	vesely-architect@sbcglobal.net

Property Owner	PV Indio, LLC	Phone Number	858-625-0100
Mailing Address	4350 La Jolla Village Dr. #110 San Diego, ca 92122	Email	kwechter@pacventures.com

### LOCAL JURISDICTION AGENCY

Local Agency Name	City of Indio	Phone Number	(760) 391-4016
Staff Contact	Rosie Lua	Email	rlua@indio.org
Mailing Address	100 Civic Center Mall Indio, CA 92201	Case Type	Design Review
Local Agency Project No	42925 Madison Street - RV Canopies <u>DR 17-09-420</u>	<input type="checkbox"/>	General Plan / Specific Plan Amendment
		<input type="checkbox"/>	Zoning Ordinance Amendment
		<input type="checkbox"/>	Subdivision Parcel Map / Tentative Tract
		<input type="checkbox"/>	Use Permit
		<input type="checkbox"/>	Site Plan Review/Plot Plan
		<input checked="" type="checkbox"/>	Other <u>Design Review</u>

### PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address	42925 Madison Street Indio, CA	Gross Parcel Size	4.49 acres
Assessor's Parcel No.	606-092-016-8, 606-092-013-5	Nearest Airport and distance from Airport	6.8 miles
Subdivision Name			
Lot Number			

### PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe)	Existing self storage and boat/RV storage facility. Owner proposes installing 7 free standing metal canopies totaling 47,918 sq. ft. to provide shade and protection for RVs/boats currently parked. Canopies will support 98 kw-DC photovoltaic solar array to provide solar power to facility.
---------------------------------	--

BDings  
Dr. C



Proposed Land Use (describe)			
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)		
For Other Land Uses (See Appendix C)	Hours of Operation	7:00 a.m. to 10:00 p.m.	
	Number of People on Site	2	Maximum Number 12
	Method of Calculation	actual counts	
Height Data	Site Elevation (above mean sea level)	18-20	ft.
	Height of buildings or structures (from the ground)	17'	ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?	<input type="checkbox"/>	Yes
		<input checked="" type="checkbox"/>	No
	If yes, describe		

- A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:**
1. . . . . Completed ALUC Application Form
  1. . . . . ALUC fee payment
  1. . . . . Plans Package (24x36 folded) (site plans, floor plans, building elevations, landscaping plans, grading plans, subdivision maps)
  1. . . . . Plans Package (8.5x11) (site plans, floor plans, building elevations, landscaping plans, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
  1. . . . . CD with digital files of the plans (pdf)
  1. . . . . Vicinity Map (8.5x11)
  1. . . . . Detailed project description
  1. . . . . Local jurisdiction project transmittal
  3. . . . . Gummed address labels for applicant/representative/property owner/local jurisdiction planner
  3. . . . . Gummed address labels of all surrounding property owners within a 300 foot radius of the project site (**only required if the project is scheduled for a public hearing Commission meeting**). If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10) with ALUC return address. \*

\* Projects involving heliports/helicopter landing sites will require additional noticing procedures.

**COUNTY OF RIVERSIDE  
AIRPORT LAND USE COMMISSION**

**STAFF REPORT**

**AGENDA ITEM:** 3.4

**HEARING DATE:** December 14, 2017

**CASE NUMBER:** ZAP1040TH17 – Coachella Green, LLC (Representative: The Altum Group)

**APPROVING JURISDICTION:** City of Coachella

**JURISDICTION CASE NO:** CZ 17-01 (Change of Zone), CUP 279 (Conditional Use Permit), AR 17-03 (Architectural Review)

**MAJOR ISSUES:** None

**RECOMMENDATION:** Staff recommends that the Commission find the proposed Change of Zone CONSISTENT with the 2005 Jacqueline Cochran Regional Airport Land Use Compatibility Plan as amended in 2006, and find the proposed Conditional Use Permit and Architectural Review CONSISTENT, subject to the conditions included herein.

**PROJECT DESCRIPTION:** The applicant proposes to change the zoning of 49.19 acres from M-H-IP (Heavy Industrial – Industrial Park Overlay) to M-S-IP (Manufacturing Service - Industrial Park Overlay). AR17-03 is a proposal to build a fourteen-building industrial complex on this site with a total building area of 644,567 square feet, anticipated to be built in five phases. CUP 279 is a proposal to allow cannabis cultivation, processing, testing, manufacturing, and distribution uses within the proposed buildings on this property. (Currently Buildings A, J, and K are proposed to be utilized for cannabis cultivation and related uses as described above. Tenants/uses for the remaining buildings have not been identified.) No dispensaries are proposed.

**PROJECT LOCATION:** The project site is located southerly of Industrial Way, easterly of Enterprise Way, northerly of Avenue 54 and westerly of the Whitewater River Storm Channel in the City of Coachella, approximately 9,100 feet northerly of the northerly terminus of Runway 17-35 at Jacqueline Cochran Regional Airport.

**LAND USE PLAN:** 2005 Jacqueline Cochran Regional Airport Land Use Compatibility Plan (last amended in 2006)

- a. Airport Influence Area: Jacqueline Cochran Regional Airport
- b. Land Use Policy: Airport Compatibility Zones C, D

c. Noise Levels: Below 55 CNEL from aircraft at this location

**BACKGROUND:**

Change of Zone: The applicant proposes to change the zoning of the property from M-H-IP (Heavy Industrial – Industrial Park Overlay) to M-S-IP (Manufacturing Service - Industrial Park Overlay). The applicant is requesting this change of zoning because the proposed cannabis cultivation and manufacturing uses are not permitted in the M-H zoning district. The IP Industrial Park Overlay Zone providing for well-planned and orderly development of industrial parks within the City was recently amended to allow cannabis-related land uses and can be applied to any property that is zoned M-S within specified districts in the City. The ordinance amendment allows for cannabis-related land uses with a Conditional Use Permit. The proposed M-S zone does allow retail and restaurant type uses, which could potentially exceed the Compatibility Zone C and D intensity criteria. However, such uses are not proposed through this project, nor have they been evaluated by ALUC staff. A condition is recommended specifying that uses be limited to offices, manufacturing, cultivation, storage, and warehousing. More intense uses such as retail sales or restaurants would require subsequent evaluation as to airport land use compatibility.

Non-Residential Average Intensity: Compatibility Zone C limits average intensity to 75 people per acre and Compatibility Zone D limits average intensity to 100 people per acre. Approximately 7.4 acres of the project site are located within Zone C, and approximately 41.7 acres are located in Zone D.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, the following rates were used to calculate the occupancy for the proposed buildings:

- Manufacturing – 1 person per 200 square feet,
- Office – 1 person per 200 square feet, and
- Storage – 1 person per 300 square feet.

Based on the project application, only Buildings A, J, and K are presently earmarked for cannabis cultivation and related uses, whereas the other buildings are speculative and do not have tenants at this time. Therefore, staff has evaluated the remaining buildings on the basis of office/industrial use at a potential occupancy of 1 person per 200 square feet. Uses with occupancies greater than one person per 100 square feet pursuant to the California Building Code should not be established therein without subsequent airport land use compatibility review.

The project proposes a total of 644,567 square feet of building area on 49.19 acres, consisting of 14 buildings divided into five separate phases and parcels. If one assumed that all building area were to be occupied on the basis of 1 person per 200 square feet, the site would accommodate 3,223 people, for an average intensity of 66 people per acre, which is consistent with Zone C average criterion of 75 people per acre and Zone D average criterion of 100 people per acre. Evaluating each Compatibility Zone separately, Zone D with a site area of 41.7 acres and a building area of 546,467

square feet would accommodate 2,732 people and Zone C with an area of 7.4 acres and a building area of 98,100 square feet would accommodate 491 people. Both areas would have an average intensity of 66 people per acre, which would be consistent with average intensity criteria for both Zone C (75 people per acre) and Zone D (100 people per acre).

Portions of Buildings J and K are located in Compatibility Zone C. Based on the floor plans for these buildings, each would include 30,812 square feet of storage area and 6,977 square feet of office manufacturing area, accommodating 136 people per building (272 people for both), for an average intensity of 37 people per acre, which is consistent with the Zone C criterion of 75 people per acre. However, only 98,100 square feet of building J and K are located within Zone C, resulting in a potential occupancy of 231 people and an average intensity of 31 people per acre.

Considering average intensity on a lot-by-lot basis (the applicant proposes to adjust parcel lines as shown on the site plan) using the ratio of 1 person per 200 square feet for each building, Phase 1 (parcel 4) results in an occupancy of 575 people, and an average intensity of 79 people per acre, Phase 2 (parcel 2) results in an occupancy of 659 people, and an average intensity of 69 people per acre, Phase 3 (parcel 3) results in an occupancy of 534 people, and an average intensity of 67 people per acre, Phase 4 (parcel 5) results in an occupancy of 792 people, and an average intensity of 63 people per acre, Phase 5 (parcel 1) results in an occupancy of 663 people, and an average intensity of 57 people per acre. When further analyzing Phase 4 (parcel 5), 98,100 square feet of building area is located within Zone C (60,300 square feet of building area is located within Zone D area of 5.25 acres), and using the applicant's proposed floorplan as identified above, the occupancy would be 231 people resulting in an average intensity of 31 people per acre in Zone C. (Zone D occupancy would be 305 people and an average intensity of 58 people per acre.) Based on a lot-by-lot basis, all phases would be consistent with the average intensity criteria.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle in the absence of more precise data). Based on the number of parking spaces provided of 1,151, the total occupancy would be 1,727 people for an average intensity of 35 persons per acre, which would be consistent with both Zone C and D average intensity criteria.

Non-Residential Single-Acre Intensity: Compatibility Zone C limits single-acre intensity to 150 people, and Compatibility Zone D limits single-acre intensity to 300 people.

Based on the applicant's proposed floor plan, single-acre areas for Buildings J and K can be identified within Compatibility Zone C. Building J includes 33,600 square feet of building area, resulting in an occupancy of 85 people (with the single acre area being located towards the middle of the building). Building K includes 33,600 square feet of building area, resulting in an occupancy of 80 people (with the single acre area being located towards the northerly end of the building). Both single-acre intensities are consistent with the Compatibility Zone C single acre criterion of 150 people. Even if each of these single-acre areas included 7,000 square feet of office space as well as 26,600 square feet of storage areas (areas that are not normally intensively manned), the single-acre

intensities would not exceed 150 persons per acre.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Zones C or D (Children's schools, day care centers, libraries, hospitals, nursing homes, buildings with more than 2 aboveground habitable floors, highly noise-sensitive outdoor non-residential uses, aboveground bulk storage of hazardous materials, critical community infrastructure facilities, and hazards to flight) within the project.

Noise: The property lies outside the areas subject to average exterior noise levels greater than 55 CNEL. Therefore, no special measures to mitigate aircraft-generated noise are required.

Part 77: The elevation of Runway 18-36 at its northerly terminus is approximately -114 feet (i.e., 114 feet below mean sea level). At a distance of approximately 9,100 feet from the runway to the project site, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding -23 feet mean sea level (MSL). The site elevation is approximately -95 feet MSL, and their maximum building height is 35 feet, for a top point elevation of -60 feet MSL. Therefore, review of buildings by the FAA Obstruction Evaluation Service (FAA OES) is not required.

Open Area: Compatibility Zone C requires a minimum of 20% open area, and Zone D requires a minimum of 10% of open area for projects 10 acres or larger be set aside as open area that could potentially serve as emergency landing areas. Based on the project size located within each Compatibility Zones, the project is required to provide a minimum of 5.65 acres of open area consistent with ALUC open area criteria (Zone C requires 1.48 acres, Zone D requires 4.17 acres). The applicant has provided 1.7 acres of open area in Zone C and 4.2 acres in Zone D for a total of 5.9 acres, largely located within the internal private streets and driveways of the project as shown on the open area exhibit. These areas are to remain as open space and the project is conditioned to maintain at least 5.65 acres of this area consistent with ALUC open area requirements of 300 feet by 75 feet minimum shape, and prohibit obstructions greater than 4 feet in height that are at least 4 inches in diameter.

## **CONDITIONS:**

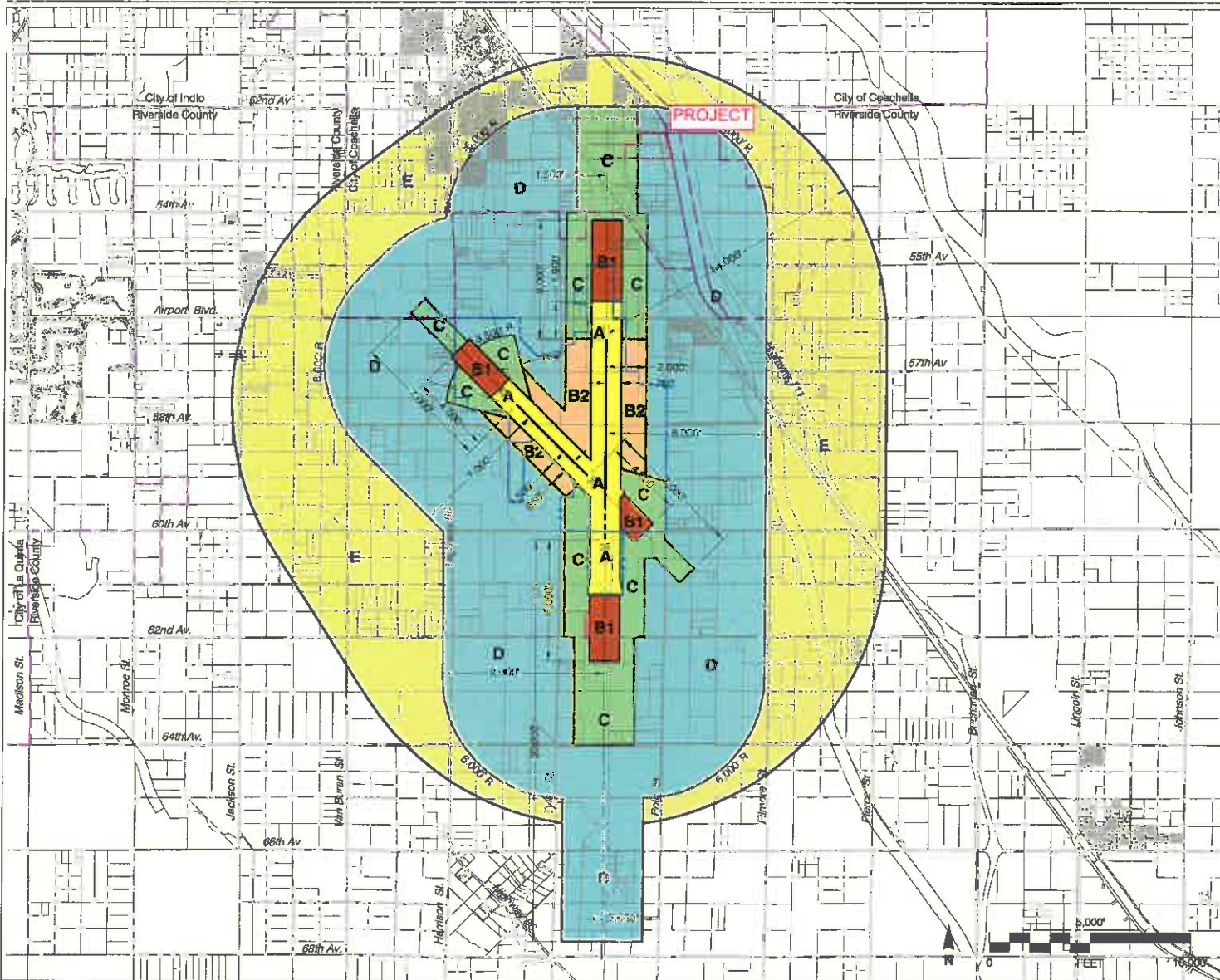
1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting plans, if any, shall be transmitted to Riverside County Economic Development Agency – Aviation Division personnel and to the Jacqueline Cochran Regional Airport for review and comment. (Failure to comment within thirty days shall be considered to constitute acceptability on the part of the airport manager.)
2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations, or any type of strobe light, toward an

aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.

- (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, livestock operations, production of cereal grains, sunflower, and row crops, artificial marshes, wastewater management facilities, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Children's schools, day care centers, libraries, hospitals, nursing homes, places of worship, highly noise-sensitive outdoor non-residential uses, and hazards to flight.
3. The attached notice shall be provided to all prospective purchasers of the property and tenants of the buildings thereon, and shall be recorded as a deed notice.
  4. Any new detention basin(s) shall be designed so as to provide a maximum 48-hour detention period for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basin that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
  5. The proposed buildings were evaluated on the basis of office, manufacturing, storage, and warehousing uses. (Storage includes indoor cultivation areas not requiring constant attention.) Within Buildings J and K, not more than 7,000 square feet of each building may be utilized for offices and manufacturing uses (other than cultivation). No dispensaries, retail uses, restaurants, or other uses with Building Code occupancy levels greater than one person per 100 square feet are authorized without subsequent airport land use compatibility evaluation.
  6. The ALUC eligible open areas shown on the open space exhibit (minimum 5.65 acres) shall be kept obstacle and obstruction free per ALUC open area definition.

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



**Legend**

**Compatibility Zones**

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E

**Boundary Lines**

- Airport Property Line - Existing
- Airport Property Line - Planned
- City Limits

**Note**

Except for southern extension, Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAA Part 77). All other dimensions measured from runway ends and centerlines.

See Chapter 2, Table 2A for compatibility criteria associated with this map.

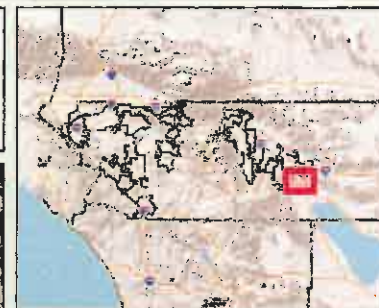
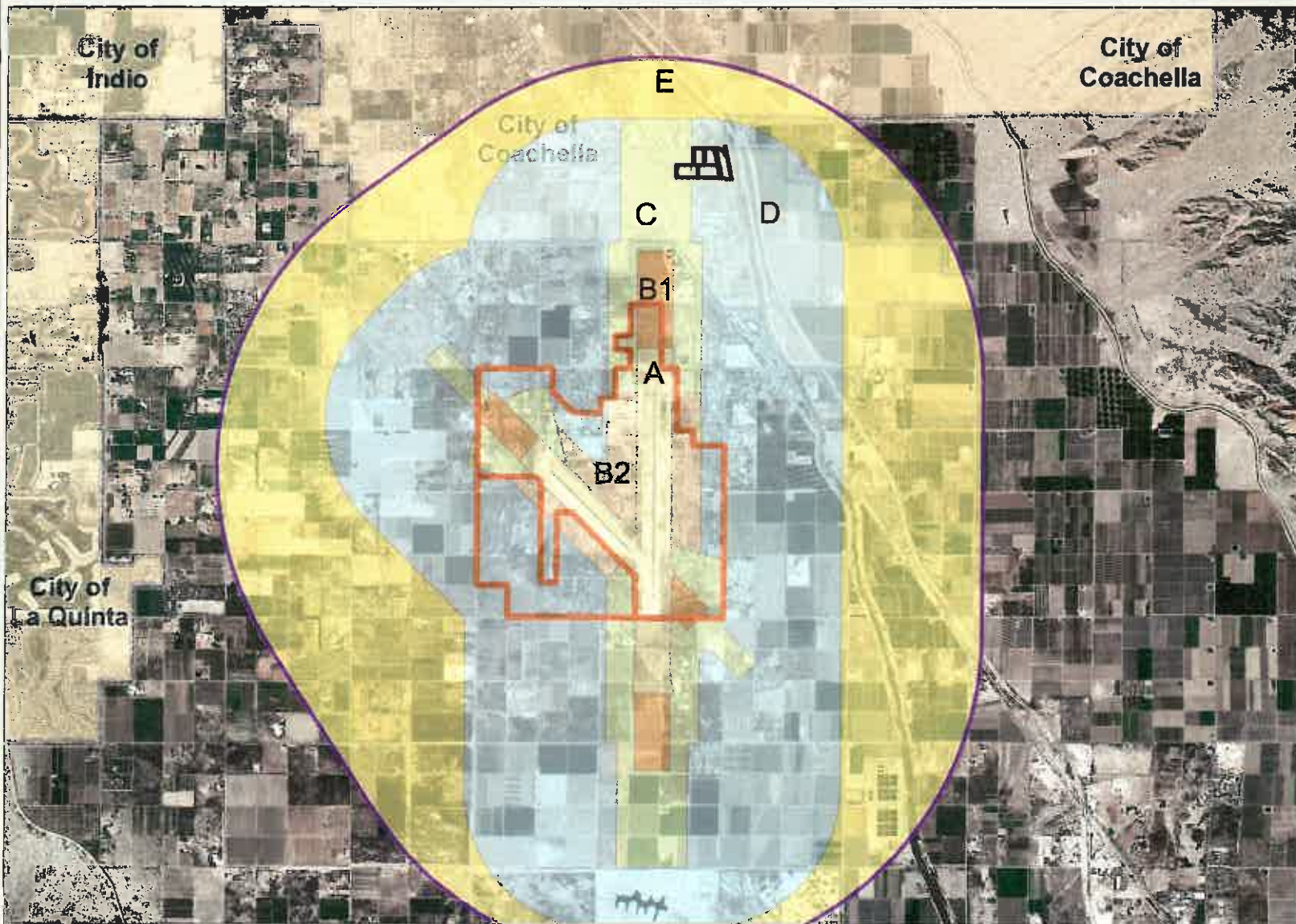
**Riverside County**  
**Airport Land Use Commission**  
**Riverside County**  
**Airport Land Use Compatibility Plan**  
**Policy Document**  
*(Adopted June 2005)*

Map JC-1

**Compatibility Map**  
**Jacqueline Cochran Regional Airport**



# My Map



## Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



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## Notes

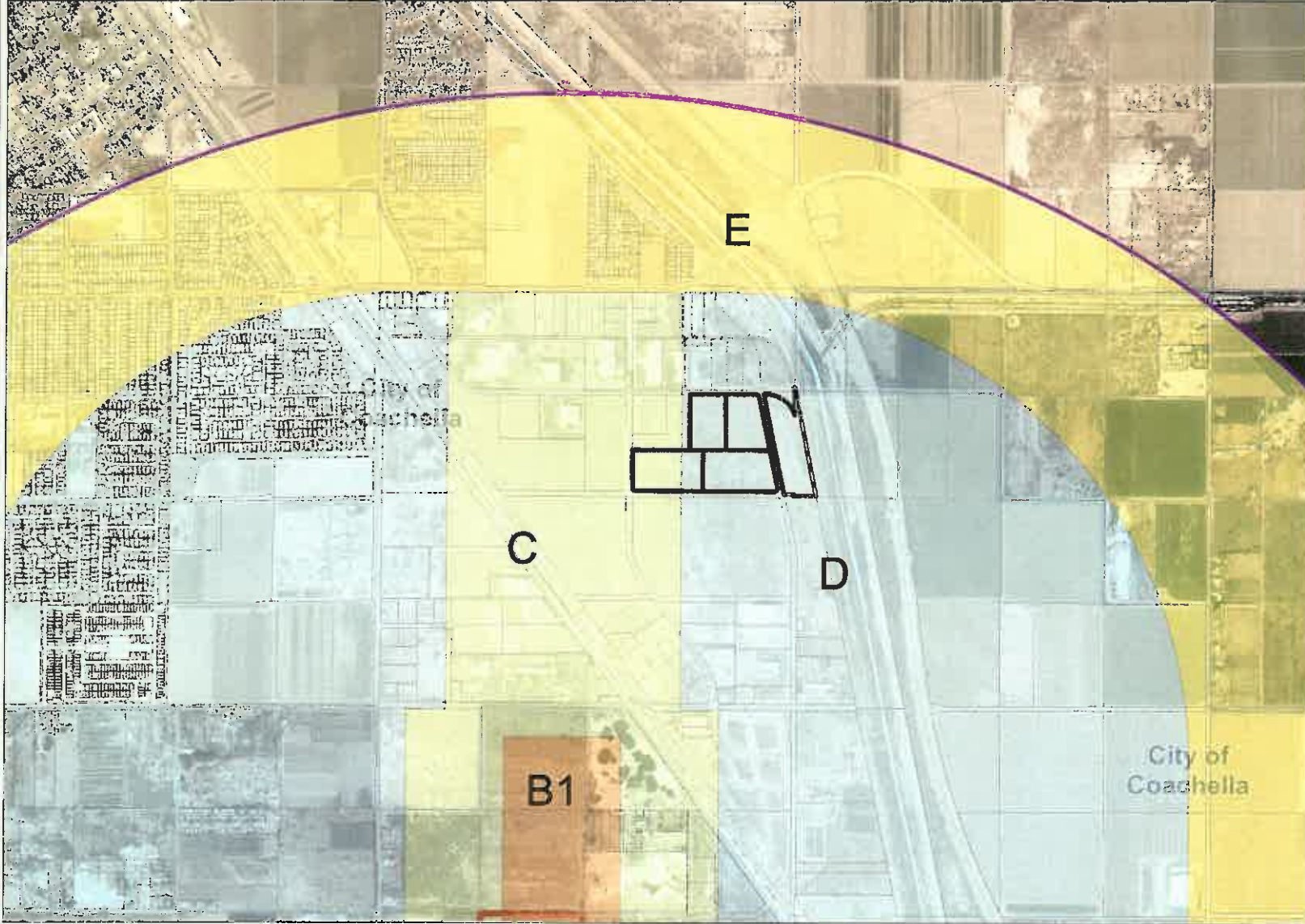
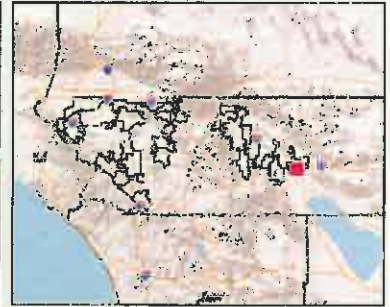
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# My Map



## Legend

- Airports
- AIA
- Airport Compatibility
- OTHER\_ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



0 2,387 4,773 Feet



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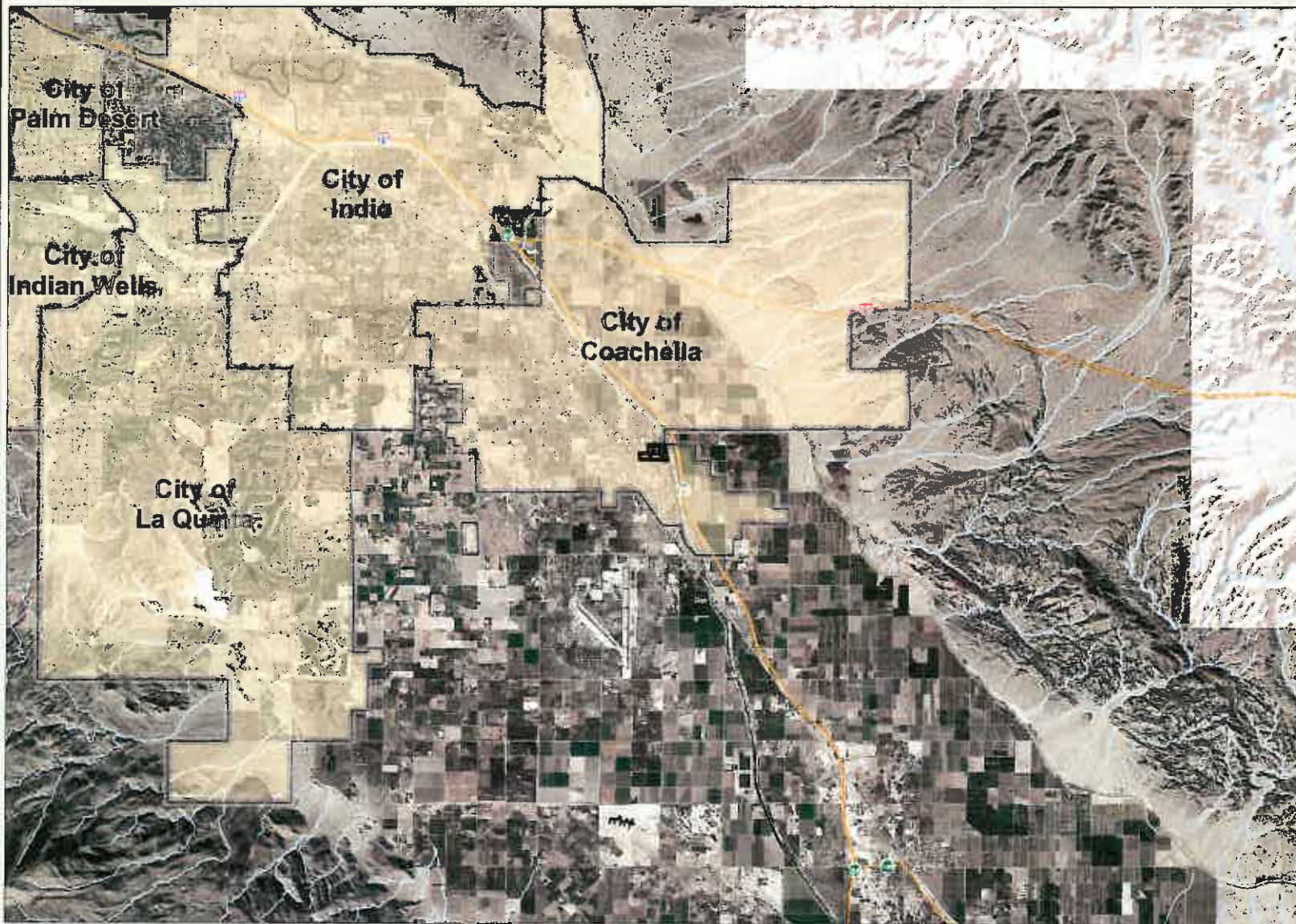
## Notes



**Project Site and Vicinity**  
**Coachella Green Industrial Park**

**Exhibit**  
**2**

# My Map



## Legend

- City Boundaries
- Cities
- adjacent\_highways**
  - Interstate
  - Interstate 3
  - State Highways; 60
  - State Highways 3
  - US HWY
  - OUT
- highways\_large**
  - HWY
  - INTERCHANGE
  - INTERSTATE
  - USHWY
- counties
- cities



0 15,768 31,536 Feet



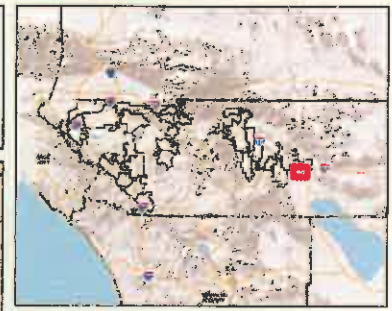
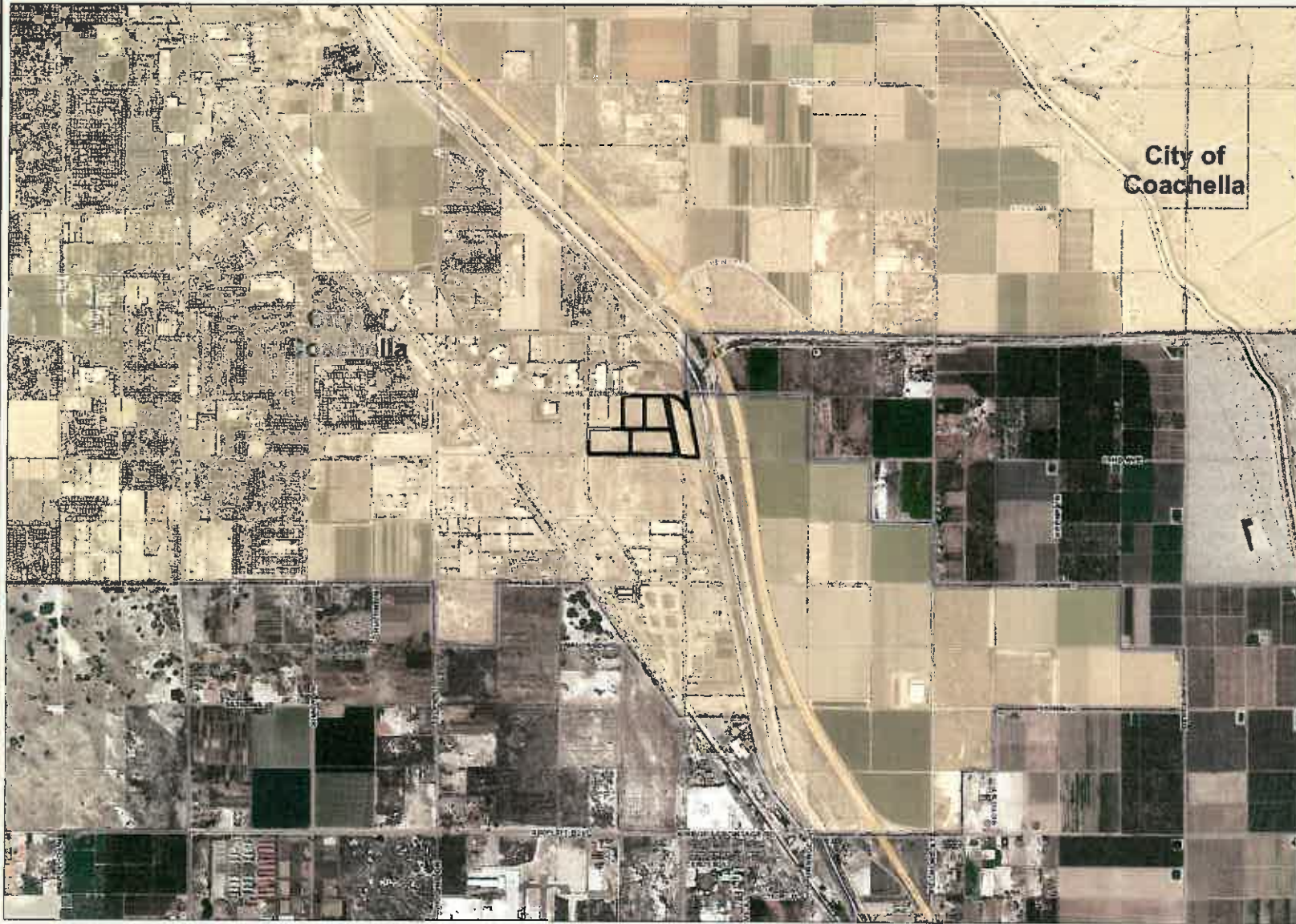
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## Notes

# My Map



## Legend

- City Boundaries
- Cities
- roads
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



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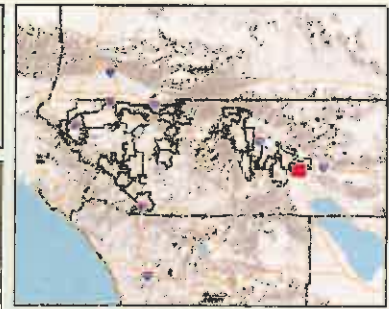
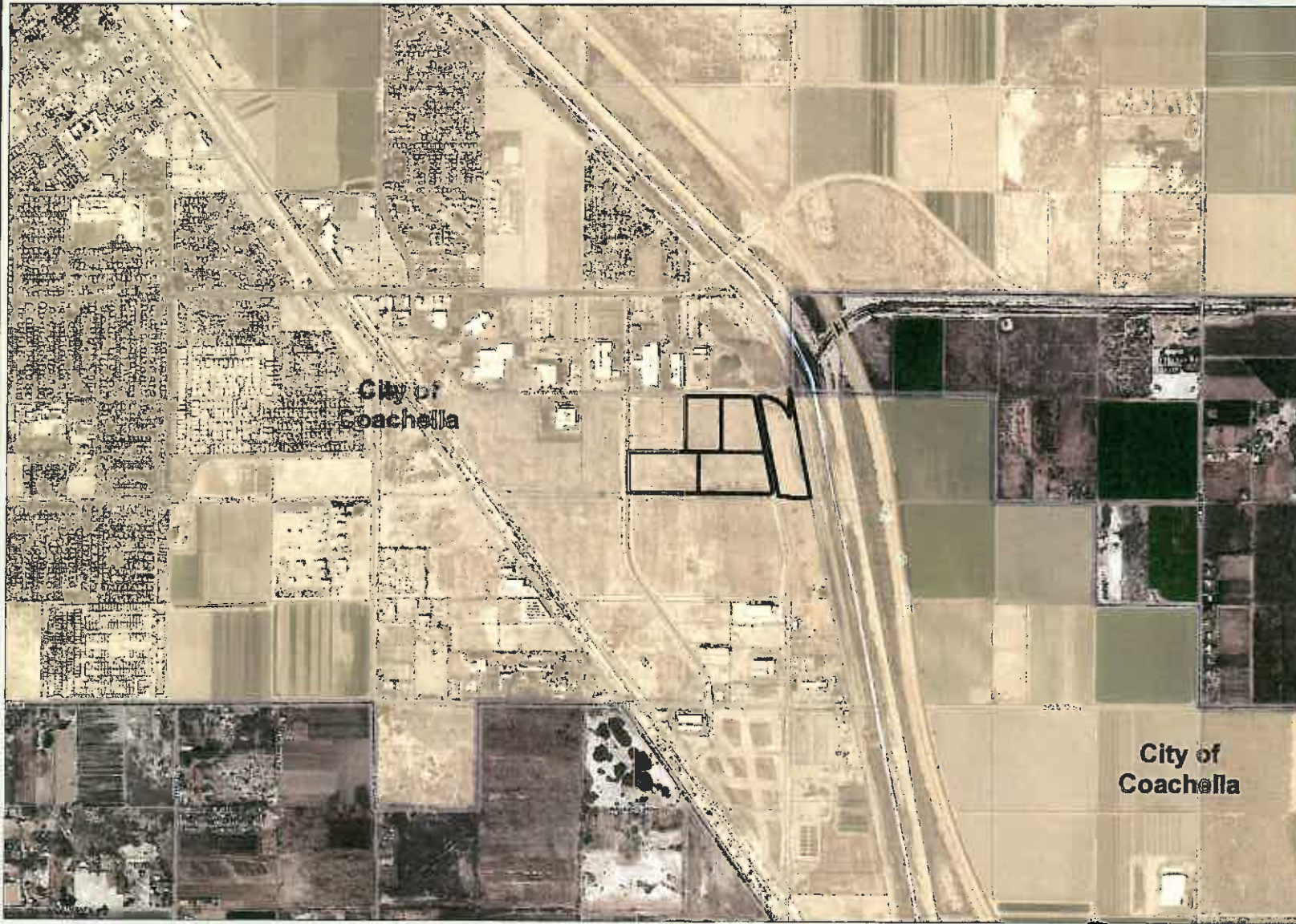
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## Notes

# My Map



## Legend

- City Boundaries
- Cities
- roads
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 2,387 4,773 Feet



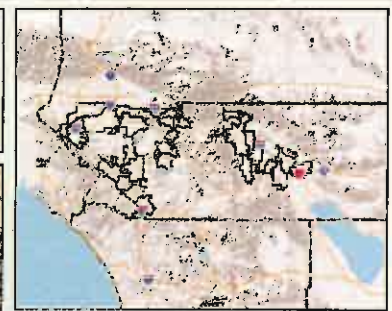
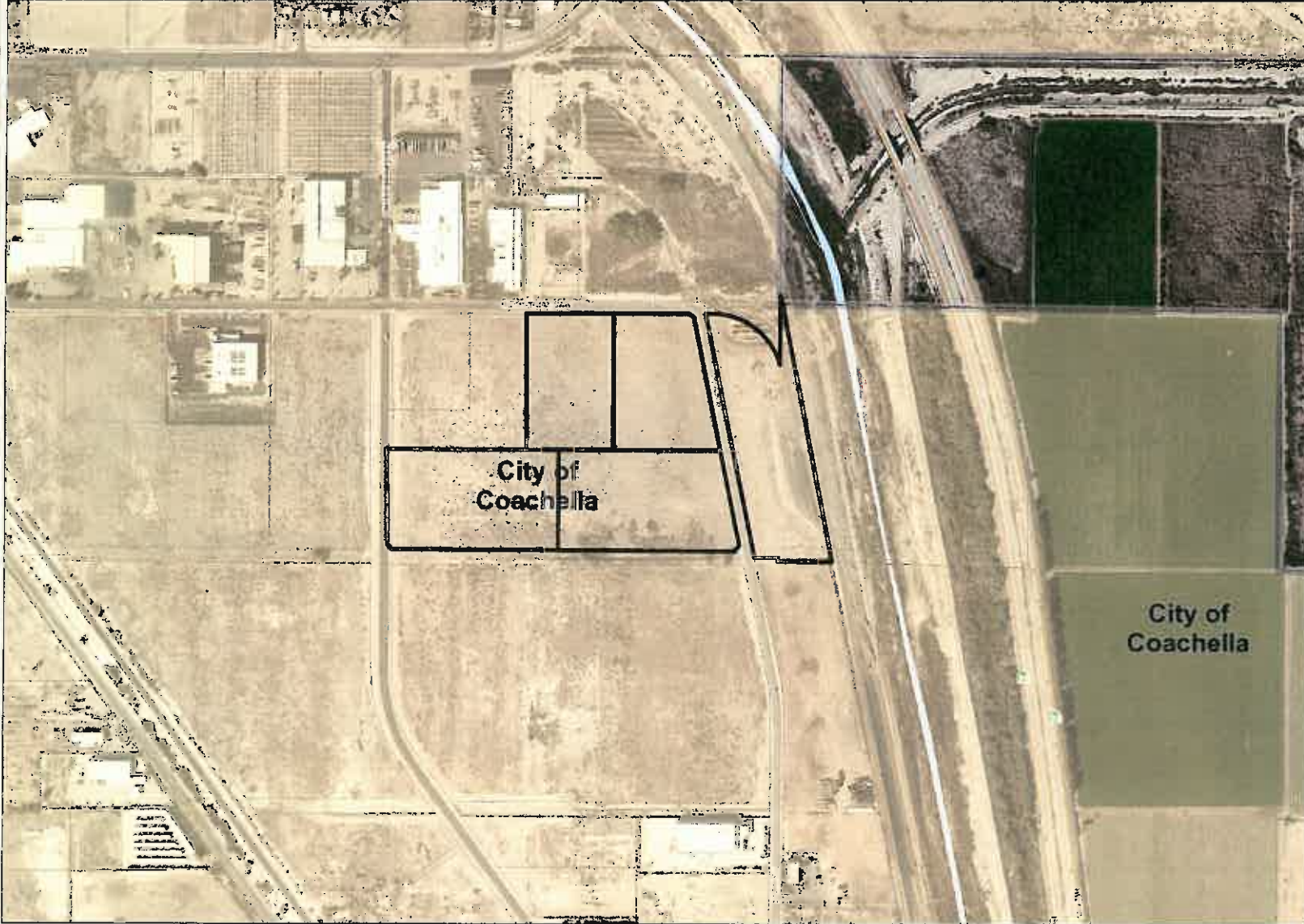
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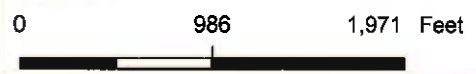
## Notes

# My Map



### Legend

- City Boundaries
- Cities
- roadsanno
- highways
  - HWY
  - INTERCHANGE
  - INTERSTATE
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  - ONRAMP
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- cities
- hydrographylines
- waterbodies
  - Lakes
  - Rivers



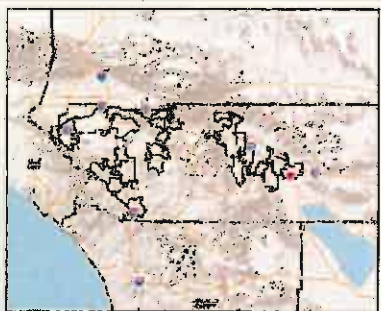
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### Notes

# My Map



## Legend

- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
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- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



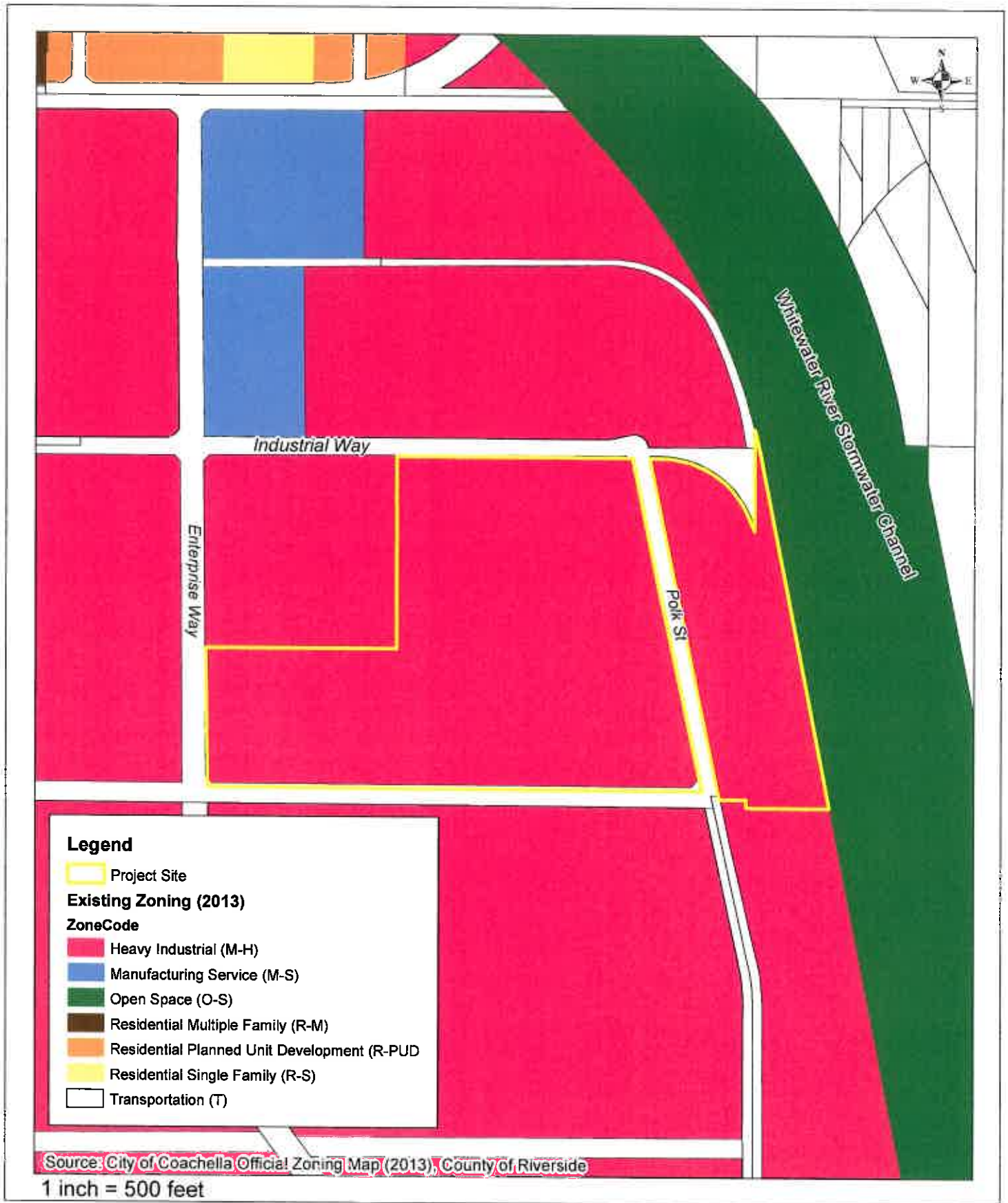
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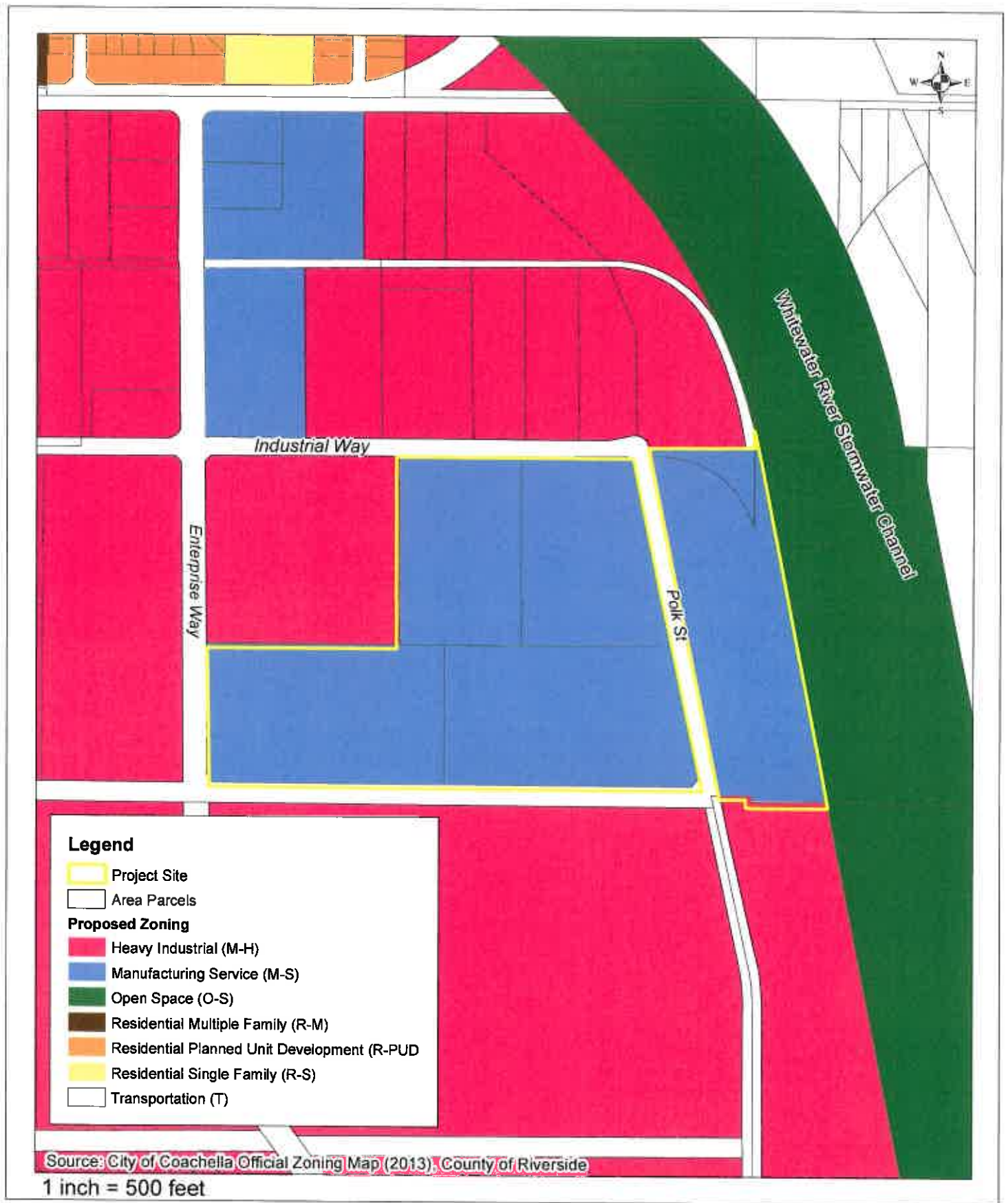
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## Notes

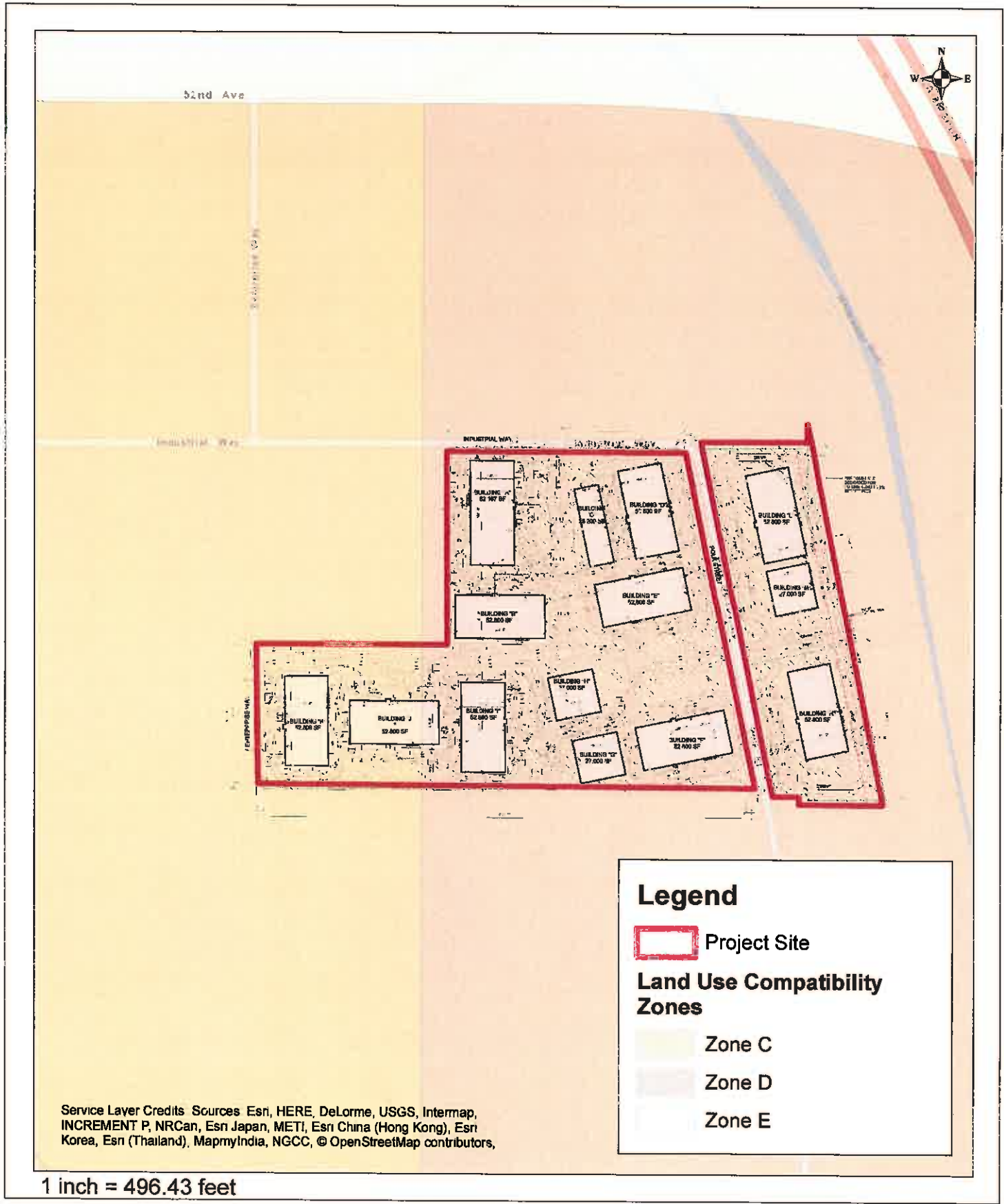




Existing Zoning  
Coachella Green Industrial Park



Proposed Zoning Designation  
 Coachella Green Industrial Park



# COACHELLA GREEN INDUSTRIAL PARK - PHASE 1

86-601 Industrial Way  
Coachella, CA 92236

## PROJECT DATA

**OWNER**  
COACHELLA GREEN, LLC  
888 S. BROADWAY, SUITE 200  
LOS ANGELES, CA 90007

**SITE ADDRESS**  
86-601 Industrial Way  
Coachella, CA 92236

**ASSESSOR'S PARCEL NO.**  
783-031-482 THROUGH 088, 089, 090, 091, 092, 093, 094, 095, 096, 097, 098, 099, 100, 101, 102, 103, 104, 105, 106, 107, 108, 109, 110, 111, 112, 113, 114, 115, 116, 117, 118, 119, 120, 121, 122, 123, 124, 125, 126, 127, 128, 129, 130, 131, 132, 133, 134, 135, 136, 137, 138, 139, 140, 141, 142, 143, 144, 145, 146, 147, 148, 149, 150, 151, 152, 153, 154, 155, 156, 157, 158, 159, 160, 161, 162, 163, 164, 165, 166, 167, 168, 169, 170, 171, 172, 173, 174, 175, 176, 177, 178, 179, 180, 181, 182, 183, 184, 185, 186, 187, 188, 189, 190, 191, 192, 193, 194, 195, 196, 197, 198, 199, 200

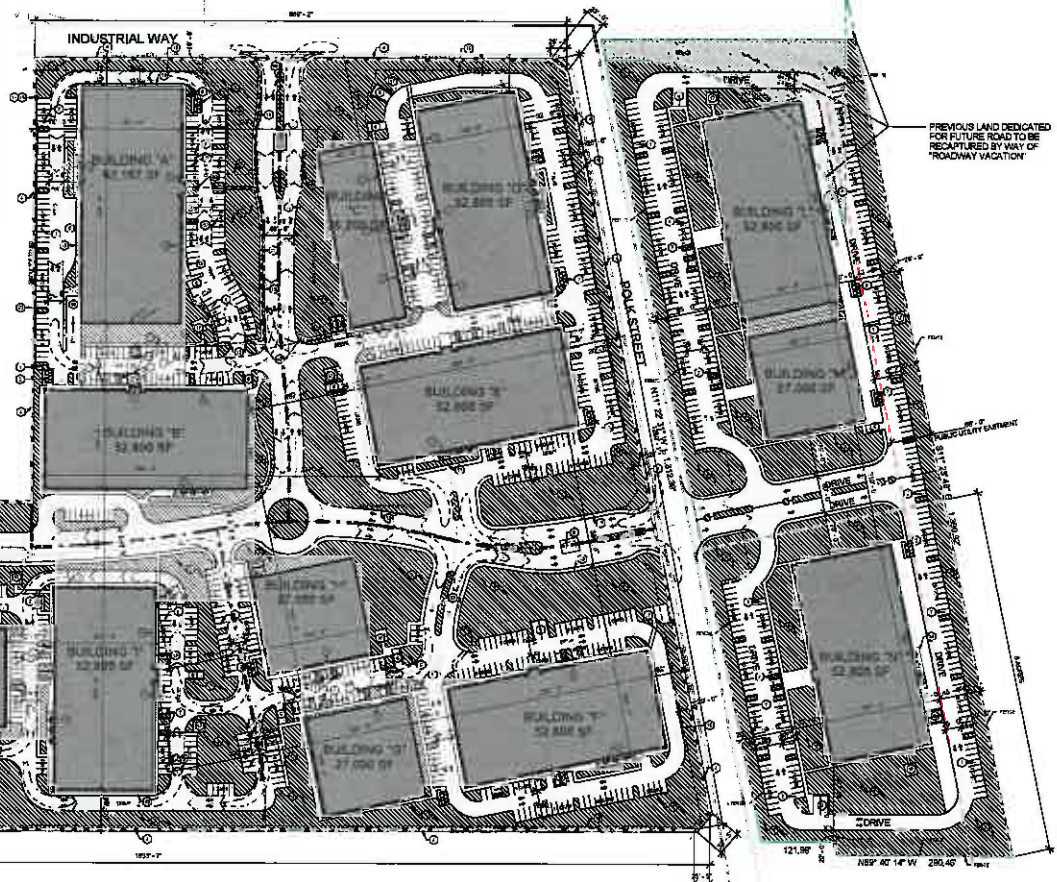
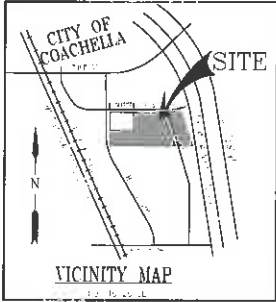
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**LAND USE INFORMATION AND OVERLAYS**  
SITE AREA: 48.18 ACRES (34.427 A.P.)  
SUBDIVISION: UNCLASD  
ZONING: M-1 (MANUFACTURING)  
HYDROLOGIC CATCHMENT: BROWN WAREHOUSE

**PHASING AREAS**  
PHASE 1: 12.28 ACRES  
PHASE 2: 12.28 ACRES  
PHASE 3: 12.28 ACRES  
PHASE 4: 12.28 ACRES  
PRELIMINARY LOT COVERAGE

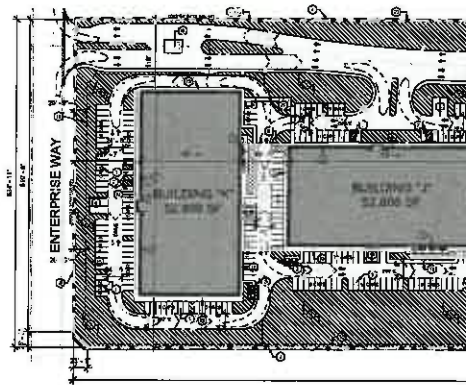
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**PHASE 2:** 12.28 ACRES  
**PHASE 3:** 12.28 ACRES  
**PHASE 4:** 12.28 ACRES

**LANDSCAPE:**  
PHASE 1: LANDSCAPE: 87,386.04 SF  
PHASE 2: LANDSCAPE: 114,225.87 SF  
PHASE 3: LANDSCAPE: 118,379.00 SF  
PHASE 4: LANDSCAPE: 118,379.00 SF  
TOTAL: 438,370.91 SF = 10% OF SITE  
PHASE 1: LANDSCAPE: 87,386.04 SF = 10% OF SITE



## LEGEND

- AREA OF LANDSCAPE**  
[Symbol]
- AREA OF PAVING**  
[Symbol]
- DIRECTIONAL ARROW**  
[Symbol]
- AREA OF FIRE LANE (MINIMUM 20' WIDTH)**  
[Symbol]
- PRE-TRUCK TURNING RADII**  
[Symbol]
- POLE MOUNT AREA LIGHT (2 HEADS) 10' - MTL, HT 20' A.F.S.**  
[Symbol]
- POLE MOUNT AREA LIGHT (2 HEADS) 10' - MTL, HT 20' A.F.S.**  
[Symbol]
- POLE MOUNT AREA LIGHT (2 HEADS) 10' - MTL, HT 20' A.F.S.**  
[Symbol]
- WALL MOUNT TYPE AREA FLOOD LIGHT, SEE EXTERIOR ELEVATIONS FOR MOUNTING HEIGHT**  
[Symbol]
- LANDSCAPE PATH OF TRAVEL, NOTE TO INCLUDE ON SLOPE IN DIRECTION OF TRAVEL AND NOT TO INCLUDE ON CROSS SLOPE**  
[Symbol]
- SITE RETAINING WALL, SEE CIVIL OR GEOTECH**  
[Symbol]
- PROPOSED LOT LINE ADJUSTMENT**  
[Symbol]
- KEY NOTES**
- 1' 0" x 2' 0" PARKING SPACES
  - TRASH ENCLOSURE SEE S41.1.3
  - LANDSCAPE
  - PROPERTY LINE
  - EXISTING TRAFFIC CORNER
  - LOADING AREA
  - ACCESSIBLE PARKING
  - SITE RECEIPTION SECURITY GUARD HOUSE, SEE DETAIL 1.241.0.2
  - MAIN ENTRANCE
  - LOADING DOOR
  - PATH OF TRAVEL
  - CURB RAMP WITH DETECTABLE WARRING
  - CONCRETE WALKWAY
  - CHILLER LOCATION
  - SEE FINISH, SEE DETAIL 7.01 ON SHEET A1.0.2 (FUTURE)
  - SEE FINISH, SEE DETAIL 1.01 ON SHEET A1.0.3 (FUTURE)
  - RECEPTION AREA
  - PLANT WITHIN RECYCLE BIN
  - MINIMUM 8" HIGH FENCE 1' WEST 10' FROM STREET, 1/2 CHAIN LINK FENCE W/ LANDSCAPE SCREENING THEREAFTER
  - 1/2 CHAIN LINK FENCE W/ LANDSCAPE

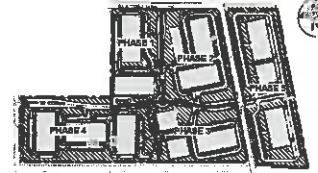


1 MASTER SITE PLAN

## PROJECT INFORMATION

BUILDING	PRIMARY BUILDING USE	SPRINKLERED	BUILDING FOOTPRINT		PERMITTED		PARCELS		PHASING		TYPE 'C' LEGALITY	
			AREA (SQ. FT.)	PERIMETER (LINEAL FT.)	PERMITTED	PERMITTED	PERMITTED	PERMITTED	PERMITTED	PERMITTED	PERMITTED	PERMITTED
PHASE 1												
A	COLLABORATION - (O) TYPE 3	Yes	62,162	1,287'-0"	83	3	83	3	3	3	3	3
B	COLLABORATION - (O) TYPE 2	Yes	82,800	1,287'-0"	83	3	83	3	3	3	3	3
			144,962		176		176					
PHASE 2												
C	COLLABORATION - (O) TYPE 3	Yes	62,162	1,287'-0"	83	3	83	3	3	3	3	3
D	COLLABORATION - (O) TYPE 2	Yes	82,800	1,287'-0"	83	3	83	3	3	3	3	3
			144,962		176		176					
PHASE 3												
E	COLLABORATION - (O) TYPE 3	Yes	62,162	1,287'-0"	83	3	83	3	3	3	3	3
F	COLLABORATION - (O) TYPE 2	Yes	82,800	1,287'-0"	83	3	83	3	3	3	3	3
			144,962		176		176					
PHASE 4												
G	COLLABORATION - (O) TYPE 3	Yes	62,162	1,287'-0"	83	3	83	3	3	3	3	3
H	COLLABORATION - (O) TYPE 2	Yes	82,800	1,287'-0"	83	3	83	3	3	3	3	3
			144,962		176		176					
PHASE 5												
I	COLLABORATION - (O) TYPE 3	Yes	62,162	1,287'-0"	83	3	83	3	3	3	3	3
J	COLLABORATION - (O) TYPE 2	Yes	82,800	1,287'-0"	83	3	83	3	3	3	3	3
			144,962		176		176					
Grand Total:			864,897		1,080		1,080					

## PHASING PLAN



**BDV**  
Charles Shapiro  
(760) 963-6000  
cal@bdv.com  
P.O. Box 643  
Coachella, CA 92233

DATE: \_\_\_\_\_ DESCRIPTION: \_\_\_\_\_ NO: \_\_\_\_\_

**MARCO STECK**  
Architecture | Engineering | Planning  
240 N. Market Pl., Escondido, CA 92029  
760.743.4100 | 760.743.4100 fax  
marco@marcosteck.com | www.marcosteck.com

**CLIENT LOGO**  
[Logo]

**CLIENT** COACHELLA GREEN, LLC

**PROJECT** COACHELLA GREEN INDUSTRIAL - PHASE 1

**ADDRESS** 86-601 Industrial Way  
Coachella, CA 92236

**PROJECT NO.** 1700

**DATE** Issue Date

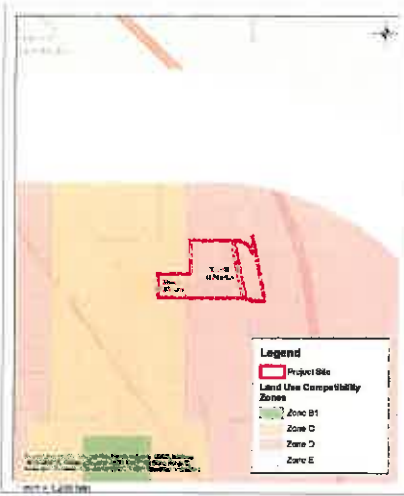
**SCALE** As Indicated

**TITLE** MASTER SITE PLAN - PHASE 1

**PHASE SHEET** 1 A1.0.0

MARCO STECK Architecture | planning 2015 ©

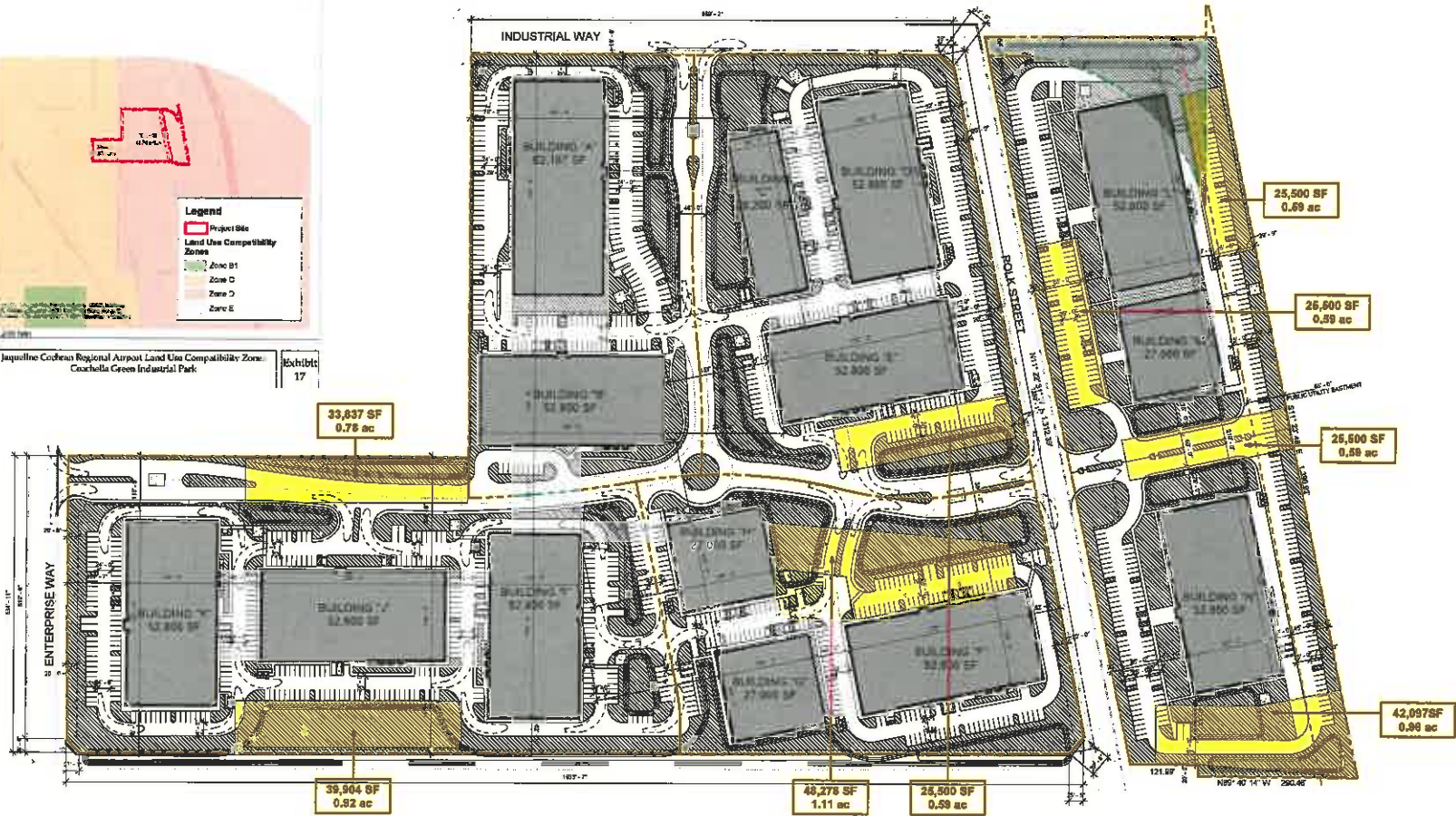
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Juqueline Cochran Regional Airport Land Use Compatibility Zone  
Coachella Green Industrial Park

Exhibit 17

Open Space Tabulation			
Zone	Acreage	Open Space Required	Open Space Provided
C	7.43	1.49 Acres ( 64,905 s.f.)	1.70 Acres ( 73,741 s.f.)
D	41.74	4.20 Acres (182,952 s.f.)	4.21 Acres (183,477 s.f.)



1 MASTER SITE PLAN - ALUC OPEN SPACES



BDN  
Civil/Structural  
(760) 825-6600  
California Architects  
P.O. Box 880  
Escondido, CA 92023

DATE	DESCRIPTION	NO.



MARCELO  
architecture | engineering | planning  
340 N. Market Pl., Escondido, CA 92023  
760.764.4100 | 760.764.4111 fax  
marcelo@marcelectra.com | www.marcelectra.com

CLIENT LOGO  
CLIENT COACHELLA GREEN, LLC  
PROJECT COACHELLA GREEN INDUSTRIAL  
ADDRESS 65-01 Industrial Way Coachella, CA 92236  
PROJECT NO. 1700

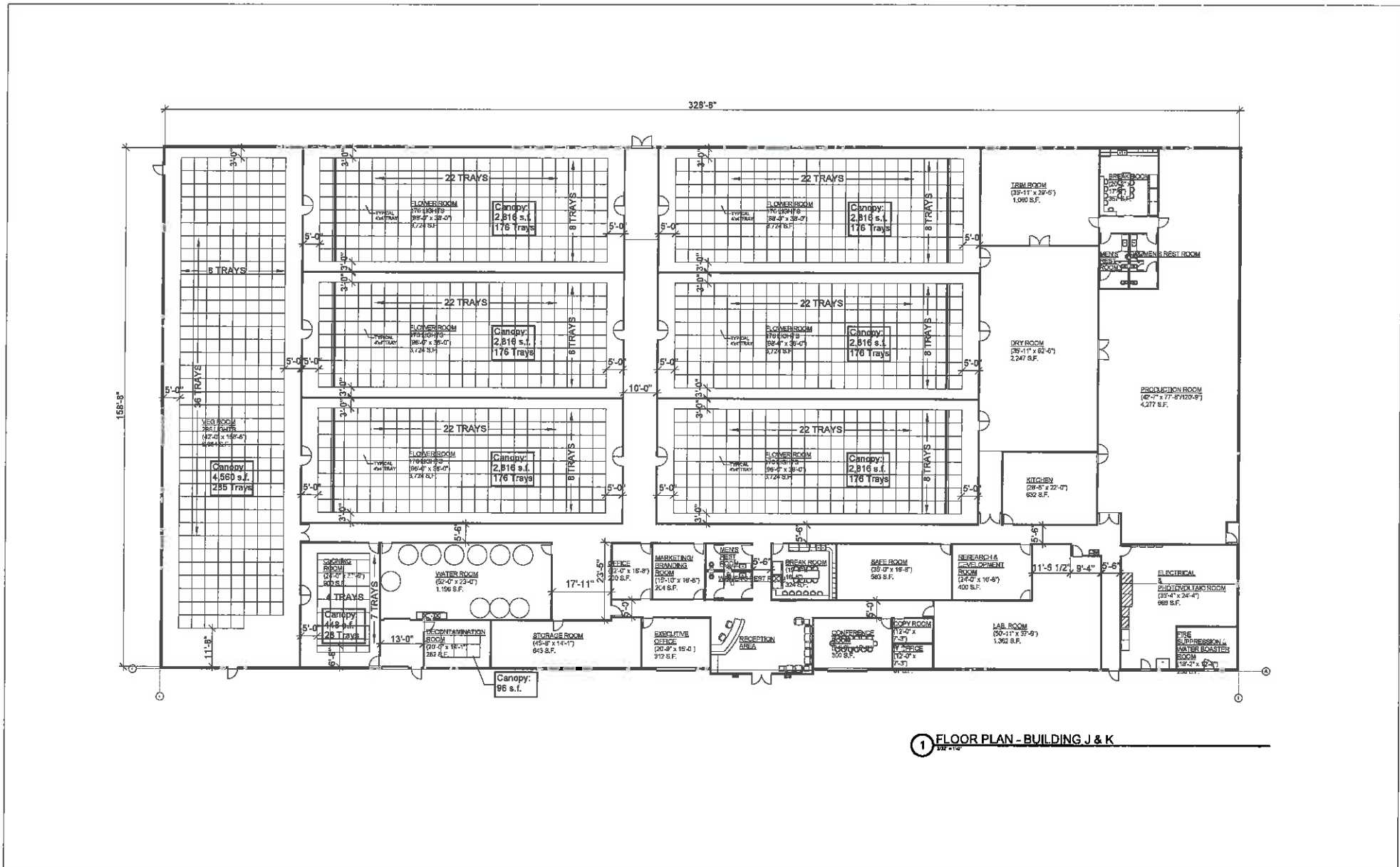
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TITLE  
MASTER SITE PLAN - ALUC OPEN SPACES

BRIEF  
A1.0.4  
MARCELO architecture | planning 2015 ©

PRINTED DATE: 10/20/15 10:27 AM





1 FLOOR PLAN - BUILDING J & K



DATE	DESCRIPTION	NO.



CLIENT LOGO  
  
 CLIENT COACHELLA GREEN, LLC  
 PROJECT COACHELLA GREEN INDUSTRIAL - PHASE 1  
 ADDRESS 66-601 Industrial Way, Coachella, CA 92236  
 PROJECT NO. 1700

DATE Issue Date  
 SCALE 3/32" = 1'-0"  
 TITLE FLOOR PLAN - BUILDING J & K  
 PHASE SHEET 1 A1.0.5  
 MARCAECHO architecture | planning 2015 ©

PRINTED DATE: 11/20/14 10:27 AM









Coachella Green  
Proposed Interim Structure

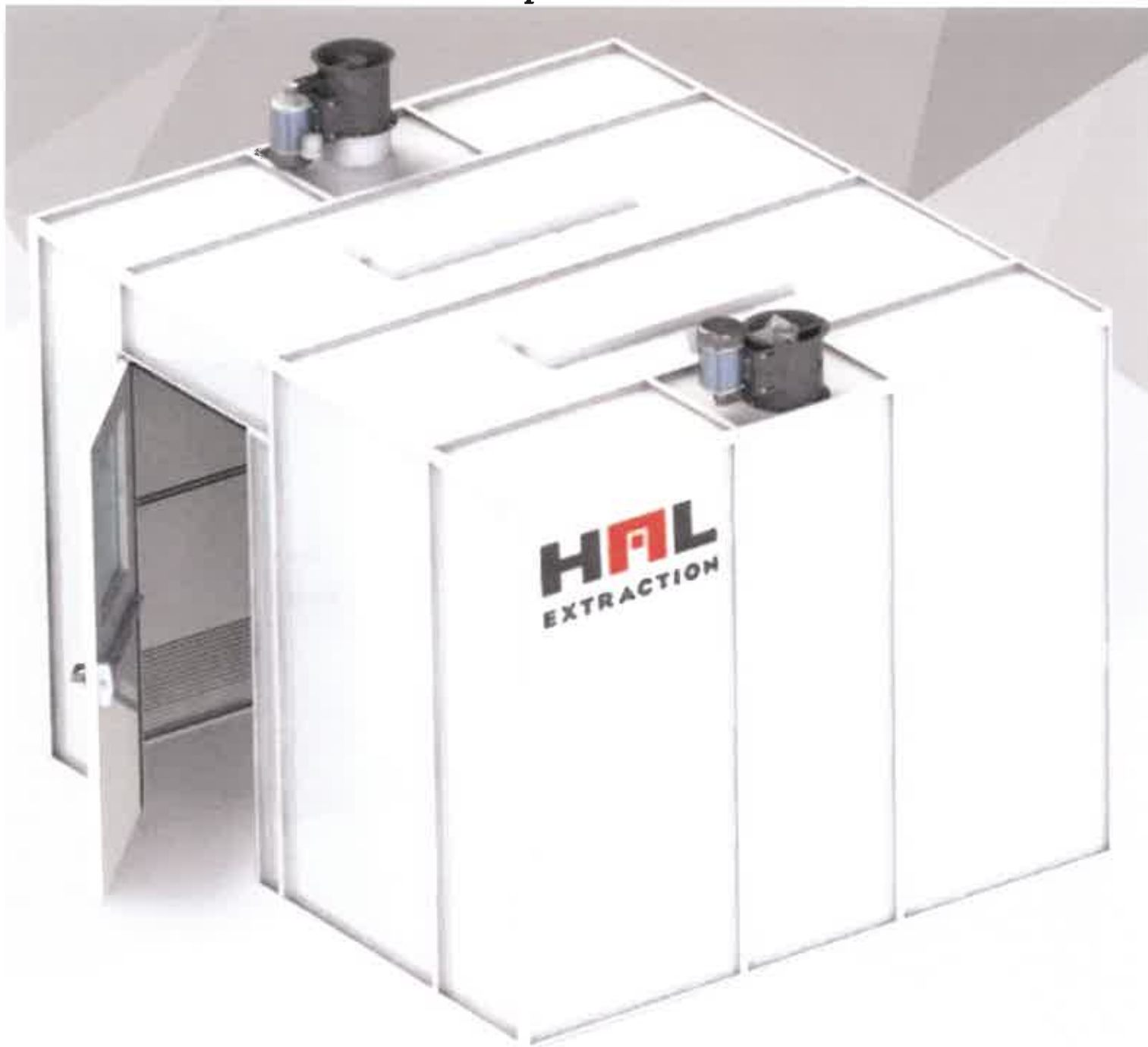


PURPOSE BUILT GREENHOUSES



COMMERCIAL **GABLE**

**Coachella Green  
Proposed Interim Structure**



BACKGROUND: CITY OF COACHELLA ORDINANCE No. 1103  
CITY OF COACHELLA ORDINANCE No. 1108

**ORDINANCE NO. 1103**

**AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF COACHELLA, CALIFORNIA AMENDING CHAPTER 17.46 OF TITLE 17 (ZONING) OF THE COACHELLA MUNICIPAL CODE IN ORDER TO AMEND THE CITY'S REGULATIONS FOR THE INDUSTRIAL PARK OVERLAY ZONE.**

**WHEREAS**, Chapter 17.46 of the Coachella Zoning Code has established the "IP Industrial Park Overlay Zone" which sets certain regulations for special uses within the City's "M-S (manufacturing service)" and "M-H (heavy manufacturing)" zones; and

**WHEREAS**, the City Council desires to amend the regulations for the "IP Industrial Park Overlay Zone" in order to (i) restrict it to the "MS" zone and eliminate it from the "M-H zone, (ii) further limit its geographical boundaries to three sub-areas under the General Plan (iii) add a minimum project area requirement, (iv) allow medical cannabis cultivation, processing, testing, manufacturing and distribution by conditional use permit, and (v) amend other development standards in the Overlay Zone; and

**WHEREAS**, the Planning Commission conducted a properly noticed public hearing on March 15, 2017 at which members of the public were afforded an opportunity to comment upon this Ordinance, the recommendations of staff and public testimony; and

**WHEREAS**, after such hearing, the Planning Commission recommended by a 3-2 vote that the City Council approve this Ordinance with modifications; and

**WHEREAS**, the City Council conducted a properly noticed public hearing on April 12, 2017 at which members of the public were afforded an opportunity to comment on this Ordinance, the recommendations of staff and public testimony.

**NOW THEREFORE, THE CITY COUNCIL OF THE CITY OF COACHELLA, CALIFORNIA, DOES ORDAIN AS FOLLOWS:**

(Deleted text is in ~~strikeout~~; additional text is in *bold italics*)

**SECTION 1.** Chapter 17.46 of Title 17 of the Coachella Municipal Code is hereby amended to read as follows:

**Chapter 17.46 - IP INDUSTRIAL PARK OVERLAY ZONE**

**17.46.010 - Intent and purpose.**

This overlay zone is intended to provide for the well-planned and orderly development of industrial parks within the community and to eliminate or adequately mitigate any adverse impacts on the community related to such types of development. The IP (industrial park) overlay zone designation may be applied to ~~any~~ *certain* property

*described herein* that is zoned M-S (manufacturing service) ~~or M-H (heavy manufacturing)~~. When the IP overlay zone designation is added to such property, the provisions of this article shall apply in addition to the regulations provided by the underlying M-S ~~or M-H~~ zone. Whenever there is a conflict or inconsistency between the provisions of this article and the underlying zone, the provisions of this chapter shall be controlling.

**17.46.015 – Property Eligible for IP Industrial Park Overlay Zone Classification.**

*The IP Industrial Park Overlay Zone classification shall be limited to those projects that are:*

- A. Located within Sub-Areas #6 (Downtown Expansion), #7 (South Employment District), or #10 (North Employment District) of the City of the Coachella General Plan; and*
- B. Zoned as M-S (manufacturing service); and*
- C. A minimum of thirty (30) acres in project area (all lots developed according to common plan or scheme).*

**17.46.020 - Permitted uses.**

- A. Except as expressly conditioned or prohibited in this chapter, ~~A~~all uses permitted in the underlying M-S ~~or M-H~~ zone shall be permitted in the IP overlay zone.;*
- B. Research and Development;*
- C. Professional Office;*
- D. Warehousing and Distribution.*

**17.46.023 – Conditional Uses.**

*The following uses may be permitted in the IP overlay zone subject to obtaining a conditional use permit as specific in Section 17.74.010:*

- A. Cannabis cultivation, processing, testing, manufacturing and/or wholesale distribution, subject to the regulatory requirements of Chapter 5.68 of this Code. Retail transaction of cannabis of any kind is prohibited in this Zone.*
  - 1. For purposes of this subsection A., “cannabis cultivation, processing, testing, manufacturing and/or wholesale distribution” shall not be deemed as the permitted uses of “drugs manufacture”, “food products processing, manufacturing, canning, preserving and freezing”, “fruit*

*and vegetable packing house”, or “testing laboratories” under Section 17.30.020(A).*

**17.46.026 – Prohibited Uses.**

*The following uses are prohibited in the IP overlay zone:*

- A. *Cannabis dispensaries or other uses engaging in the retail sale, exchange or distribution of cannabis;***
- B. *Outdoor storage yard;***
- C. *Automobile repair business;***
- D. *Automobile body and fender works;***
- E. *Any other conditional uses in the M-S Zone, except as set forth in this chapter.***

**17.46.030 - Property development standards.**

- A. *Project Area/Lot Requirements.***
  - 1. *Minimum Project Area. Thirty acres. For purposes of this paragraph, “project area” shall mean the combined area of all legally subdivided lots developed as a common plan or scheme by the same or affiliated developer(s).***
  - 2. *Minimum individual Lot Size. One Five (5) acres for any lot on which is located a cannabis cultivation, processing, testing, manufacture or distribution use. For all other lots, one (1) acre.***
  - 23. *Minimum Lot Width. One hundred eighty (180) feet.***
  - 34. *Minimum Lot Depth. Two hundred twenty (220) feet.***
  - 45. *Maximum Lot Coverage. Fifty (50) percent. The Development Services Director may allow individual lots within a project area to exceed this standard if he or she finds that: (i) it will result in more orderly development of the project area and (ii) the average lot coverage of all lots within the project area does not exceed fifty (50) percent.***
  - 6. *No project subject to this Chapter shall use more than sixty-five percent (65%) of the project area for cannabis cultivation, processing, testing, manufacture or distribution uses.***



B. Front Yard Requirements.

1. Twenty-five (25) feet from the curb on all property fronting on Avenue 54 and Avenue 52.
2. Twenty (20) feet from the curb on all property fronting Industrial Way, Enterprise Way or Polk Street.
3. Ten (10) *feet* from the curb on all property fronting any local street.
4. All front setbacks shall be landscaped in a manner approved by the planning commission.
5. No buildings, facilities or other improvements shall be allowed in a required front yard except for landscaping or block entryways approved by the planning commission. Notwithstanding the foregoing, required yards may be used for automobile parking; provided, that landscaping approved by the planning commission is provided along the frontage of the property.

C. Heights Limits. The maximum height of any building or structure shall be fifty (50) feet.

D. Distance Between *Uses/Buildings*. *No cannabis cultivation, processing, testing, manufacture or distribution use shall be located within 1,000 feet of any residentially zoned lot. The distance shall be measured at the nearest point between any part of the building containing the cannabis use and any lot line of the residential use.*

E. On-Street/Off-Street Parking and Loading.

1. Off-Street Parking and Loading. Off-street parking and loading facilities shall be provided in accordance with the provisions of Section 17.54.010 of this title.
2. On-Street Parking and Loading. On-street parking or loading shall be prohibited.

F. Walls and Screening.

1. Each development on a lot or parcel of property shall be enclosed by ~~fencing or other type of screening approved~~ *with decorative masonry walls and/or wrought iron fencing, subject to review* by the planning commission.
2. ~~All walls, fences or other approved screening shall not be less than six feet in height. Notwithstanding the foregoing, block wall entryways approved~~

~~by the planning commissions shall not exceed forty two (42) inches in height in a required yard. *parking lots and loading areas shall be screened from view to the street with low decorative masonry walls and landscaping, subject to review by the Planning Commission.*~~

~~G. Road Standards.~~

~~1. Collector roads (Enterprise Way and Polk Street) shall consist of:~~

~~a. Right of way of sixty four (64) feet;~~

~~b. Curb to curb road width of forty eight (48) feet; and~~

~~e. Dedicated parkway strip of eight feet.~~

~~2. Interior local roads shall consist of:~~

~~a. Fifty eight (58) feet of right of way;~~

~~b. Forty two (42) feet curb to curb; and~~

~~e. Eight feet dedicated parkway strip.~~

**HG. Other Property Development Standards.**

1. All utilities shall be underground, until such time as the power transmitted is greater than thirty-four (34) KV and then it shall be brought to the attention of the planning commission prior to any construction.
2. All developments shall include an exterior lighting system to provide adequate are security. Such lighting system shall use high-pressure sodium lights or an equivalent type of light approved by the planning commission.
3. All developments shall be landscaped in a manner approved by the planning commission.

**17.46.040 - Architectural review.**

All developments shall be subject to architectural review in accordance with Section 17.72.010 of this title, *including planning commission review of design guidelines and signs as described below*. Any addition to or exterior alteration of an existing development also shall be subject to such architectural review.

- A. ***Design Guidelines. Detailed architectural and/or artist renderings, or pictorial depictions, of allowable building types, architectural themes, and typical common-area landscaping and lighting shall be provided for all industrial park overlay zone developments. Conceptual design guidelines in text describing allowable architectural theming and exterior building materials, roofing and roof line treatments, window and door opening designs, and landscaping plant palettes, must accompany the industrial park overlay zone architectural review submittal. The use of green houses, butler buildings, or other pre-fabricated structures are prohibited unless they are treated architecturally to have the appearance of a contemporary concrete tilt-up building with parapet roof structures. All main buildings shall have variation in exterior wall planes and roof lines to limit massing and monotone design features, subject to review by the planning commission.***
- B. ***Signs. A comprehensive sign program shall be included as a part of architectural review for new industrial park overlay zone developments.”***

**SECTION 2.** Effective Date. This ordinance shall take effect thirty (30) days after its adoption.

**SECTION 3.** Severability. If any section, subsection, subdivision, paragraph, sentence, clause or phrase of this ordinance, or any part thereof is for any reason held to be unconstitutional, such decision shall not affect the validity of the remaining portion of this ordinance or any part thereof. The City Council hereby declares that it would have passed each section, subsection, subdivision, paragraph, sentence, clause or phrase thereof, irrespective of the fact that any one or more section, subsection, subdivision, paragraph, sentence, clause or phrase be declared unconstitutional.

**SECTION 4.** Certification. The City Clerk shall certify the passage of this ordinance and shall cause the same to be entered in the book of original ordinances of said City; shall make a minute passage and adoption thereof in the records of the meeting at which time the same is passed and adopted; and shall, within fifteen (15) days after the passage and adoption thereof, cause the same to be published as required by law, in a local newspaper of general circulation and which is hereby designated for that purpose.

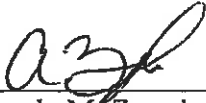
**SECTION 5.** CEQA. The City Council finds that this Ordinance is not subject to the California Environmental Quality Act (CEQA) pursuant to Sections 15060(c)(2) (the activity will not result in a direct or reasonably foreseeable indirect physical change in the environment) and 15060(c)(3) (the activity is not a project as defined in Section 15378) of the CEQA Guidelines, California Code of Regulations, Title 14, Chapter 3, because it has no potential for resulting in physical change to the environment, directly or indirectly.

**PASSED, APPROVED and ADOPTED** this 26th day of April, 2017.



\_\_\_\_\_  
Steven A. Hernandez  
Mayor

**ATTEST:**



\_\_\_\_\_  
Angela M. Zepeda  
City Clerk

**APPROVED AS TO FORM:**



\_\_\_\_\_  
Carlos Campos  
City Attorney

STATE OF CALIFORNIA        )  
COUNTY OF RIVERSIDE     ) ss.  
CITY OF COACHELLA        )


I **HEREBY CERTIFY** that the foregoing Ordinance No. 1103 was duly and regularly introduced at a meeting of the City Council on the 12<sup>th</sup> day of April, 2017, and that thereafter the said ordinance was duly passed and adopted at a regular meeting of the City Council on the 26<sup>th</sup> day of April, 2017.

**AYES:**            Councilmember Bautista, Councilmember Sanchez, Councilmember Perez, Mayor Pro Tem Martinez and Mayor Hernandez.

**NOES:**            None.

**ABSENT:**        None.

**ABSTAIN:**       None.

  
\_\_\_\_\_  
Andrea J. Carranza  
Deputy City Clerk

**ORDINANCE NO. 1108**

**AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF COACHELLA, CALIFORNIA, AMENDING CHAPTERS 17.26, 17.46, AND 17.85 OF TITLE 17 (ZONING) OF THE COACHELLA MUNICIPAL CODE REGARDING COMMERCIAL CANNABIS ACTIVITY ZONING.**

**WHEREAS**, pursuant to the authority granted to the City of Coachella (“City”) by Article XI, Section 7 of the California Constitution, the City has the police power to regulate the use of land and property within the City in a manner designed to promote public convenience and general prosperity, as well as public health, welfare, and safety; and,

**WHEREAS**, adoption and enforcement of comprehensive zoning regulations and other land use regulations lies within the City’s police power; and

**WHEREAS**, on October 9, 2015, Governor Brown signed the “Medical Cannabis Regulation and Safety Act” (“MCRSA”) into law; and

**WHEREAS**, MCRSA became effective January 1, 2016 and contains provisions which allow for local governments to regulate licenses and certain activities thereunder; and

**WHEREAS**, California has adopted, through ballot initiative, the Control, Regulate, and Tax Adult Use of Marijuana Act (“AUMA”) to regulate the adult personal and commercial use of marijuana, pursuant to local authority preserved in AUMA; and

**WHEREAS**, Chapter 17.46 of the Coachella Zoning Code has established the “IP Industrial Park Overlay Zone” which sets certain regulations for special uses within the City’s “M-S (manufacturing service)” and M-H (heavy manufacturing)” zones; and

**WHEREAS**, the City Council recently amended the regulations for the “IP Industrial Park Overlay Zone” to allow medical cannabis cultivation, processing, testing, manufacturing, and distribution by conditional use permit; and

**WHEREAS**, the City Council desires to remove the requirement in Chapter 17.46 that prohibits cannabis projects in the “IP Industrial Park Overlay Zone” that use more than sixty-five percent (65%) of the gross floor area for cannabis cultivation, processing, testing, distribution, and manufacturing uses; and

**WHEREAS**, Chapter 17.85 of the Coachella Zoning Code has established regulations on certain medical cannabis activities and businesses in the City’s “M-W (wrecking yard)” zone; and

**WHEREAS**, the City Council desires to amend the regulations in Chapter 17.85 in order to (i) to conform to current City policies and state law, (ii) allow both medicinal

and recreational adult use cannabis cultivation, processing, testing, manufacturing, and distribution by conditional use permit, (iii) allow cannabis testing laboratories and certain distribution dispatch offices to operate in the "C-G (general commercial)" zone with a conditional use permit, and (iv) amend other development standards regarding commercial cannabis activity; and

**WHEREAS**, the City Council also desires to amend Chapter 17.26 of the Coachella Zoning Code to specifically allow commercial cannabis testing laboratories in the C-G zone as a conditional use such that Chapters 17.26 and 17.85 are consistent; and

**WHEREAS**, the Planning Commission conducted a properly noticed public hearing on May 17, 2017 at which members of the public were afforded an opportunity to comment upon this Ordinance, the recommendations of staff and public testimony; and

**WHEREAS**, after such hearing, the Planning Commission recommended that the City Council approve this Ordinance; and

**WHEREAS**, the City Council conducted a properly noticed public hearing on June 14, 2017 at which members of the public were afforded an opportunity to comment on this Ordinance, the recommendations of staff and public testimony.

**NOW THEREFORE, THE CITY COUNCIL OF THE CITY OF COACHELLA, CALIFORNIA DOES ORDAIN AS FOLLOWS:**

**SECTION 1. Amendment to Chapter 17.26 of Zoning Code.** Sub-Section 34 of Section 17.26.020(c) is hereby added as follows:

"34. Commercial cannabis testing laboratories, pursuant to Chapter 17.85."

**SECTION 2. Amendment to Chapter 17.46 of Zoning Code.** Sub-Section 6 of Section 17.46.030(A) is hereby deleted in its entirety.

**SECTION 3. Amendment to Chapter 17.85 of Zoning Code.** Chapter 17.85 of Title 17 of the Coachella Municipal Code is hereby amended to read as follows:

"Chapter 17.85 - COMMERCIAL CANNABIS ACTIVITY

17.85.010 - Purpose and intent.

It is the purpose and intent of this chapter to regulate the cultivation, manufacturing, testing, distribution, and transportation of medicinal and nonmedicinal cannabis (including cannabis products and edible cannabis products) within the City of Coachella. Delivery of cannabis within the city is prohibited. Additionally, cannabis dispensaries and retail sales are prohibited in the city.

The regulations in this chapter are enacted to ensure the health, safety, and welfare of the residents of the city. These regulations, in compliance with the Compassionate Use Act of 1996 (“CUA”), the Medical Marijuana Program (“MMP”), the Medical Cannabis Regulation and Safety Act (“MCRSA”), and the Control, Use, Tax Adult Use of Marijuana Act (“AUMA”) (collectively, “State Law”), do not interfere with the use and possession of cannabis, as authorized under State Law.

Nothing in this chapter shall be construed to: (1) allow persons to engage in conduct that endangers others or causes a public nuisance or (2) allow any activity relating to the cultivation, manufacturing, testing, distribution, transportation or consumption of cannabis that is otherwise illegal under California state law.

#### 17.85.020 -- Definitions.

Unless the particular provision or context otherwise requires, the definitions and provisions contained in this section shall govern the construction, meaning, and application of words and phrases used in this chapter:

“Adult use conditional use permit” or “adult use CUP” means a conditional use permit issued under this chapter for cannabis or cannabis products that are intended for adults 21 years of age or over who do not possess a physician’s recommendation.

“Applicant” means a person who is applying for a CUP under this chapter.

“Cannabis” means all parts of the plant *Cannabis sativa* Linnaeus, *Cannabis indica*, or *Cannabis ruderalis*, whether growing or not; the seeds thereof; the resin, whether crude or purified, extracted from any part of the plant; and every compound, manufacture, salt, derivative, mixture, or preparation of the plant, its seeds, or resin. “Cannabis” also means the separated resin, whether crude or purified, obtained from cannabis. “Cannabis” also means marijuana as defined by Section 11018 of the California Health and Safety Code. “Cannabis” does not include the mature stalks of the plant, fiber produced from the stalks, oil or cake made from the seeds of the plant, any other compound, manufacture, salt, derivative, mixture, or preparation of the mature stalks (except the resin extracted therefrom), fiber, oil, or cake, or the sterilized seed of the plant which is incapable of germination. For the purpose of this chapter, “cannabis” does not mean “industrial hemp” as defined by Section 81000 of the California Food and Agricultural Code or Section 11018.5 of the California Health and Safety Code.

“Cannabis products” has the same meaning as marijuana products in Section 11018.1 of the California Health and Safety Code. When the term “cannabis” is used in this chapter, it shall include “cannabis products.”



“City manager” means the city manager of the City of Coachella or designee.

“Commercial cannabis activity” includes cultivation, possession, manufacture, processing, storing, laboratory testing, labeling, transportation and distribution of cannabis, and cannabis products. For purposes of this chapter, “commercial cannabis activity” does not include delivery, dispensing, or retail sale of cannabis or cannabis products.

“Conditional use permit” means a conditional use permit issued under this chapter and includes both an adult use conditional use permit and a medicinal use conditional use permit. “Conditional use permit” may be abbreviated as “CUP”.

“Cultivation” means any commercial activity involving the planting, growing, harvesting, drying, curing, grading, or trimming of cannabis.

“Delivery” means the commercial transfer of cannabis or cannabis products to a customer.

“Distribution” means the procurement, wholesale sale, and transport of cannabis and cannabis products between entities permitted or licensed under this chapter, another local California jurisdiction, or State Law.

“Edible cannabis product” means manufactured cannabis that is intended to be used, in whole or in part, for human consumption, including, but not limited to, chewing gum, but excluding products set forth in Division 15 of the California Food and Agricultural Code. An edible cannabis product is not considered food as defined by Section 109935 of the California Health and Safety Code or a drug as defined by Section 109925 of the California Health and Safety Code. When the term “cannabis” is used in this chapter, it shall include “edible cannabis products.”

“Manufacture” means to compound, blend, extract, infuse or otherwise make or prepare a cannabis product.

“Medicinal use conditional use permit” or “medicinal use CUP” means a CUP issued under this chapter for cannabis or cannabis products that are intended for use solely by an individual who possesses a physician’s recommendation.

“Permittee” means any person or entity holding a CUP under this chapter, regardless of whether it is an adult use CUP or a medicinal use CUP.

“Person” includes any individual, firm, co-partnership, joint venture, association, corporation, Limited Liability Company, estate, trust, business trust, receiver, syndicate, or any other group or combination acting as a unit, and the plural as well as the singular.

“Retailer” means a person or entity that sells cannabis or cannabis products to customers. The term “retailer” shall also include the term “dispensary,” as defined under MCRSA.

“Testing laboratory” means a laboratory, facility, or entity in California, that offers or performs tests of cannabis or cannabis products and that is both of the following: (1) Accredited by an accrediting body that is independent from all other persons involved in commercial cannabis activity in the state; and (2) Licensed by the California Bureau of Marijuana (or Cannabis) Control within the California Department of Consumer Affairs (when such licenses begin to be issued).

“Transportation” means transferring cannabis and/or cannabis products from one person or entity permitted under this chapter, permitted by another local California jurisdiction, and/or licensed under State Law to another person or entity permitted under this chapter, permitted by another local California jurisdiction, and/or licensed under State Law.

Words and phrases not specifically defined in this code shall have the meaning ascribed to them as defined in the following sources:

- A. CUA (California Health and Safety Code Section 11362.5);
- B. MMP (California Health and Safety Code Sections 11362.7 through 11362.83);
- C. MCRSA (California Business and Professions Code Sections 19300 et seq.); and
- D. AUMA (California Business and Professions Code Sections 26000 et seq.).

**17.85.030 - Commercial cannabis activity permitted.**

Commercial cannabis activity permitted under this chapter includes cultivation, manufacture, distribution, testing, and transportation (including possession, processing, storing, and labeling incidental to such activity). Prior to engaging in any such commercial cannabis activity in the city, one must obtain a conditional use permit (CUP) pursuant to this chapter, and a regulatory permit as required by this code, subject to the

provisions of the CUA, MMP, MCRSA, AUMA and any other state laws pertaining to cannabis. Cannabis dispensaries, deliveries, and retailers are prohibited in all city zones.

17.85.040 - Conditional use permit required.

Prior to initiating operations and as a continuing requisite to operating a commercial cannabis activity, the applicant wishing to operate the above-listed commercial cannabis activity shall obtain and maintain a validly issued adult use CUP and/or medicinal use CUP from the city pursuant to chapter 17.74 of this code. If any provision of this chapter conflicts with any provision of chapter 17.74 of this code, the provision in this chapter shall control. An applicant must obtain a separate CUP for each commercial cannabis activity the applicant wishes to operate. Each CUP will include a condition of approval requiring that the permittee also obtain and maintain a cultivation, manufacture, distribution, testing, and/or transportation regulatory permit required by this code.

17.85.050 - Commercial cannabis activity —Permitted locations and standards.

A. Commercial cannabis activity may be located in any wrecking yard zone (M-W) or Industrial Park Overlay Zone (IP) in the city, upon issuance of a CUP and a regulatory permit.

B. Commercial cannabis activity in the M-W zone should be restricted to a site having a minimum of five acres in size, with a minimum paved street frontage of two hundred fifty (250) feet. Commercial cannabis activity in the IP zone is restricted to sites having a minimum project area of thirty (30) acres and a minimum lot size or grouping of lots of at least five (5) acres.

C. Commercial cannabis activity must be served by municipal water and sewer services.

D. Commercial cannabis activity shall be located a minimum distance of six hundred (600) feet away from any residentially-zoned lot. The distance shall be measured at the nearest point between any part of the building containing the cannabis use and any lot line of the residential use.

E. Commercial cannabis activity may not be established in the M-W zone on a multi-tenant industrial park or business park site existing on the effective date of this ordinance. A CUP to develop a new stand-alone commercial cannabis activity facility or a multi-tenant facility within a minimum site area of five (5) acres may be pursued.

F. Commercial cannabis activity shall not result in the creation of any odors detectable from anywhere off the property boundaries. The use of carbon filtration

systems and other mitigation measures shall be used on all commercial cannabis activities that cause such odors.

G. Testing laboratories may be located in the General Commercial zone (C-G) in addition to the M-W zone and IP overlay zone with a CUP, but are not required to meet the two hundred fifty (250) foot paved street frontage requirement in subsection (B) or the restrictions set forth in subsections (D) and (E) of this section.

**17.85.060 – State law compliance.**

A. No commercial cannabis activity shall operate unless it is in possession of all applicable state and local licenses or permits, except as otherwise permitted by state and/or local law.

B. Every commercial cannabis activity shall submit to the city manager a copy of any and all of its state and local licenses and permits required for its operation.

C. If any other applicable state or local license or permit for a commercial cannabis activity is denied, suspended, modified, revoked, or expired, the permittee shall notify the city manager in writing within ten (10) calendar days.

**17.85.070 – Payment of taxes.**

All commercial cannabis activity is required to pay all applicable taxes, including a cannabis business tax pursuant to chapter 4.31.

**17.85.080 – Prohibited operations.**

Any commercial cannabis activity that does not have both a CUP and a regulatory permit required under this code is expressly prohibited in all city zones and is hereby declared a public nuisance that may be abated by the city and is subject to all available legal remedies, including but not limited to civil injunctions. Cannabis delivery, dispensaries, and retailers are prohibited in all city zones.

**17.85.090 – Commercial cannabis cultivation—Interior only.**

All commercial cannabis cultivation shall be conducted only in the interior of enclosed structures, facilities and buildings, and all cultivation operations, including all cannabis plants, at any stage of growth, shall not be visible from the exterior of any structure, facility or building containing cultivation. All cultivation must take place indoors, within a permanent structure that is enclosed on all sides. Outdoor cultivation is prohibited. Portable greenhouses and/or non-permanent enclosures shall not be used for cultivation unless they are placed inside of a permanent structure that is enclosed on all sides.

17.85.100 - Penalties for violations.

A. In addition to any other remedy allowed by law, any person who violates a provision of this chapter is subject to criminal sanctions, civil actions, and administrative penalties.

B. Violations of this chapter constitute an infraction or misdemeanor and may be enforced by any applicable law.

C. Violations of this chapter are hereby declared to be public nuisances.

D. Each person is guilty of a separate offense each day a violation is allowed to continue and every violation of this chapter shall constitute a separate offense and shall be subject to all remedies.

E. All remedies prescribed under this chapter shall be cumulative and the election of one or more remedies shall not bar the city from the pursuit of any other remedy for the purpose of enforcing the provisions hereof.”


**SECTION 4. Effective Date.** This ordinance shall take effect thirty (30) days after its adoption.

**SECTION 5. Severability.** If any section, subsection, subdivision, paragraph, sentence, clause or phrase of this ordinance, or any part thereof is for any reason held to be unconstitutional, such decision shall not affect the validity of the remaining portion of this ordinance or any part thereof. The City Council hereby declares that it would have passed each section, subsection, subdivision, paragraph, sentence, clause or phrase thereof, irrespective of the fact that any one or more section, subsection, subdivision, paragraph, sentence, clause or phrase be declared unconstitutional.


**SECTION 6. Certification.** The City Clerk shall certify the passage of this ordinance and shall cause the same to be entered in the book of original ordinances of said City; shall make a minute passage and adoption thereof in the records of the meeting at which time the same is passed and adopted; and shall, within fifteen (15) days after the passage and adoption thereof, cause the same to be published as required by law, in a local newspaper of general circulation and which is hereby designated for that purpose.

**SECTION 7. CEQA.** The City Council finds that this Ordinance is not subject to the California Environmental Quality Act (CEQA) pursuant to Sections 15060(c)(2) (the activity will not result in a direct or reasonably foreseeable indirect physical change in the environment) and 15060(c)(3) (the activity is not a project as defined in Section 15378) of the CEQA Guidelines, California Code of Regulations, Title 14, Chapter 3, because it has no potential for resulting in physical change to the environment, directly or indirectly.


**PASSED, APPROVED and ADOPTED** this 12<sup>th</sup> day of July, 2017.

  
\_\_\_\_\_  
Steven A. Hernandez  
Mayor

**ATTEST:**

  
\_\_\_\_\_  
Angela M. Zepeda  
City Clerk

**APPROVED AS TO FORM:**

  
\_\_\_\_\_  
Carlos Campos  
City Attorney

STATE OF CALIFORNIA            )  
COUNTY OF RIVERSIDE        ) ss.  
CITY OF COACHELLA            )

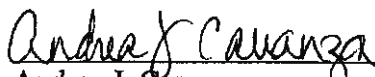
**I HEREBY CERTIFY** that the foregoing Ordinance No. 1108 was duly and regularly introduced at a meeting of the City Council on the 14<sup>th</sup> day of June, 2017, and that thereafter the said ordinance was duly passed and adopted at a regular meeting of the City Council on the 12<sup>th</sup> day of July, 2017.

**AYES:**            Councilmember Bautista, Councilmember Sanchez, Councilmember Perez, and Mayor Hernandez.

**NOES:**            None.

**ABSENT:**         Mayor Pro Tem Martinez

**ABSTAIN:**       None.

  
\_\_\_\_\_  
Andrea J. Carranza  
Deputy City Clerk

# **NOTICE OF PUBLIC HEARING**

## **RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the application described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. The proposed project application may be viewed at the Riverside County Administrative Center, 4080 Lemon Street, 14<sup>th</sup> Floor, Riverside, California 92501, Monday through Thursday from 8:00 a.m. to 5:00 p.m., except November 23 (Thanksgiving Day), and by prescheduled appointment on Fridays, from 9:00 a.m. to 5:00 p.m., except November 24.

ATTENTION: ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan. The City of Coachella will hold hearings on this project and should be contacted on non-ALUC issues.

PLACE OF HEARING:                   Riverside County Administration Center  
  4080 Lemon St, 1<sup>st</sup> Floor Board Chambers  
  Riverside, California

DATE OF HEARING:   December 14, 2017

TIME OF HEARING:   9:00 A.M.

### **CASE DESCRIPTION:**

ZAP1040TH17 – Coachella Green, LLC (Representative: The Altum Group) – City of Coachella Planning Case Nos. CZ 17-01 (Change of Zone), CUP 279 (Conditional Use Permit), AR 17-03 (Architectural Review). The applicant proposes to change the zoning of 49.19 acres located southerly of Industrial Way, easterly of Enterprise Way, northerly of Avenue 54 and westerly of the Whitewater River Storm Channel from M-H-IP (Heavy Industrial – Industrial Park Overlay) to M-S-IP (Manufacturing Service - Industrial Park Overlay). AR17-03 is a proposal to build a fourteen-building industrial complex on this site with a total building area of 644,567 square feet, anticipated to be built in five phases. CUP 279 is a proposal to allow cannabis cultivation, processing, testing, manufacturing, and distribution uses within the proposed buildings on this property. (Currently Buildings A, J, and K are proposed to be utilized for cannabis cultivation and related uses as described above. Tenants/uses for the remaining buildings have not been identified.) No dispensaries are proposed. (Airport Compatibility Zones C and D of the Jacqueline Cochran Regional Airport Influence Area)

FURTHER INFORMATION: Contact Paul Rull at (951) 955-6893. The ALUC holds hearings for local discretionary permits within the Airport Influence Areas, reviewing for aeronautical safety, noise and obstructions. All other concerns should be addressed to Mr. Luis Lopez of the City of Coachella Planning Department at (760) 398-3102.



**APPLICATION FOR MAJOR LAND USE ACTION REVIEW**  
**RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION**

ALUC Identification No.

**ZAP1040TH17**

**PROJECT PROPONENT (TO BE COMPLETED BY APPLICANT)**

Date of Application June 26, 2017

Property Owner Coachella Green, LLC Phone Number \_\_\_\_\_

Mailing Address 1933 S. Broadway, Suite 806  
Los Angeles, CA 90007

---

Agent (if any) THE ALTUM GROUP, ATTN: ANDREY NICKERSON Phone Number 760.346.4750

Mailing Address 73-710 FRED WARING DR., SUITE 219  
PALM DESERT CA 92260

JCR  
C+D

**PROJECT LOCATION (TO BE COMPLETED BY APPLICANT)**

*Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways*

Street Address 86-601 Industrial Way  
Coachella, CA 92236

Assessor's Parcel No. 763-131-082 through -086 Parcel Size 49.19 acres

Subdivision Name Coachella Green Industrial Park Zoning Classification Heavy Industrial (M-H)

Lot Number N/A

**PROJECT DESCRIPTION (TO BE COMPLETED BY APPLICANT)**

*If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed*

Existing Land Use (describe) Currently, the project site is vacant. The site is desert in character, covered by sand and ruderal vegetation. The entire site has a zoning classification of Heavy Industrial (M-H). The site is designated Industrial District in the City's 2035 General Plan.

Proposed Land Use (describe) The applicant proposes a five-phase industrial development with a total building area of 644,567 square feet. Currently, only Building A is identified for a specific use, cannabis cultivation. The remaining 13 buildings will be leased for permitted or conditionally permitted uses. The project will require a change of zone will be required by the City from M-H (Heavy Industrial) to M-S (Manufacturing Service) to allow the proposed use. To allow for cannabis cultivation practices on the project site, the applicant has requested that the Overlay Zone be applied, which is permitted within the M-S zone.

For Residential Uses Number of Parcels or Units on Site (exclude secondary units) N/A

For Other Land Uses Hours of Use 24 hours, 7 days per week

(See Appendix C) Number of People on Site Maximum Number 297

Method of Calculation 580,110 SF Cultivation (10,000 sf/4 persons)= (580,110/10,000)\*4= 232 persons;  
64,457 SF Extraction (1,000 sf/1person) = (64,457/1,000)\*1= 65 person ; TOTAL 297 employees

Height Data Height above Ground or Tallest Object (including antennas and trees) 35 ft.

Highest Elevation (above sea level) of Any Object or Terrain on Site (Max elev -94 ft) = -59 ft.

Flight Hazards Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?  Yes  No

If yes, describe \_\_\_\_\_

**REFERRING AGENCY (APPLICANT OR JURISDICTION TO COMPLETE)**

Date Received	_____	Type of Project	
Agency Name	City of Coachella	<input type="checkbox"/> General Plan Amendment	
Staff Contact	Luis Lopez, Development Services Director	<input checked="" type="checkbox"/> Zoning Amendment or Variance	
Phone Number	(760) 398-3102	<input type="checkbox"/> Subdivision Approval	
Agency's Project No.	62 17-01 AR 17-03 CUP 279	<input type="checkbox"/> Use Permit	
		<input type="checkbox"/> Public Facility	
		<input checked="" type="checkbox"/> Other	Conditional Use Permit

**A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.

**B. SUBMISSION PACKAGE:**

**ALUC REVIEW**

- 1. . . . . Completed Application Form
- 1. . . . . Project Site Plan – Folded (8-1/2 x 14 max.)
- 1. . . . . Elevations of Buildings - Folded
- 1 Each . 8 ½ x 11 reduced copy of the above
- 1. . . . . 8 ½ x 11 reduced copy showing project in relationship to airport.
- 1 Set . Floor plans for non-residential projects
- 4 Sets . Gummed address labels of the Owner and representative (*See Proponent*).
- 1 Set . Gummed address labels of all property owners within a 300' radius of the project site. If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10), with ALUC return address.
- 4 Sets . Gummed address labels of the referring agency (City or County).
- 1. . . . . Check for Fee (See Item "C" below)

**STAFF REVIEW (Consult with ALUC staff planner as to whether project qualifies)**

- 1. . . . . Completed Application Form
- 1. . . . . Project Site Plans – Folded (8-1/2 x 14 max.)
- 1. . . . . Elevations of Buildings - Folded
- 1. . . . . 8 ½ x 11 Vicinity Map
- 1 Set . Gummed address labels of the Owner and representative (*See Proponent*).
- 1 Set . Gummed address labels of the referring agency.
- 1. . . . . Check for review—See Below

## **Chapter 1 Introduction**

### **1.1 Overview**

The applicant, Coachella Green, LLC is proposing to develop the Coachella Green Industrial Park, a commercial cannabis cultivation and manufacturing complex in the City of Coachella. The project site includes: 1) a 37.46 acre area in four parcels located at 86-601 Industrial Way, at the southwest corner of Industrial Way and Polk Street; and 2) an 11.73-acre parcel located on the southeast corner of Industrial Way and Polk Street. The project site is approximately 49.19 acres in size, not including the Polk Street right-of-way that bisects the site.

Commercial Cannabis cultivation, mixed use (retail/office), and cannabis products manufacturing are proposed within the project site, requiring the following entitlements: 1) Change of zone from M-H (Heavy Industrial) to M-S (Manufacturing Service) to allow the proposed use; 2) Master Conditional Use Permit (CUP) to allow cannabis-related land uses throughout the project site; 3) Architectural Review for the Interim Cannabis Cultivation and Manufacturing uses in the Phase 2 and 4 areas; 4) Architectural Review for each development within Phase 1 -5; 5) Lot Line Adjustment to reconfigure the lots in Phase 1 - 4; 6) Right-of-Way road vacation at the NE corner of Phase 5 (SE corner of Industrial Way and Polk Street); and 7) a Water Supply Assessment.

The project site is located in an area that is sporadically developed with a mix of industrial and agricultural uses. The project site is currently vacant and has not been previously developed. The Whitewater Storm Channel is located east of the project site and vacant land designated for industrial use is located south and west of the project site. There are a number of packing facilities located along the north side of Industrial Way; and a Coca Cola facility to the west, mid-block along Industrial Way.

#### **Project Site History**

The project site consists of five parcels that are part of a larger 6-parcel map (TPM 36858) approved by the City of Coachella in 2015. The parcel map was included in a previous Initial Study/Mitigated Negative Declaration that evaluated the potential environmental effects of subdividing the site into 6 parcels, and developing parcel 6, located at the southeast corner of Industrial Way and Enterprise Way as a new parking lot for the Ocean Mist Packing and developing a Distribution Plant located north of PM 36858 at the northeast corner of Industrial Way and Enterprise Way. The Mitigated Negative Declaration covered the mitigation measures identified for the Ocean Mist Farms Expansion Project as there was no development proposed for parcels 1-5 of TPM 36858. Development of Parcels 1 through 5 are the subject of this Initial Study.

**COUNTY OF RIVERSIDE  
AIRPORT LAND USE COMMISSION**

**STAFF REPORT**

**AGENDA ITEM:** 3.5

**HEARING DATE:** December 14, 2017

**CASE NUMBER:** ZAP1039TH17 – Thermal Operating Company, LLC  
(Representative: Albert A. Webb Associates)

**APPROVING JURISDICTION:** County of Riverside

**JURISDICTION CASE NO:** SPA00303A4 (Specific Plan Amendment), CZ07952 (Change of Zone)

**MAJOR ISSUES:** The applicant had previously been granted a consistency finding to permit utilization of “founders’ lots” located in Zone D, plus 39 units in Zone C, as residences. The allowance in Zone C is based on applying the one dwelling unit per five acre maximum residential allowance pursuant to Table 2A of the Countywide Policies to the entire area northerly of Avenue 62 located in Zone C originally included in the overall Thermal Club project. This Specific Plan Amendment proposes to allow overnight stays at the remaining 110 to 116 “founders’ lots” in Zone C by creating a new category, “racetrack recreational units.”

The applicant contends that these “racetrack recreational units” would be distinguished from residences, and should not be counted as such, because these units would be subject to a project limitation within its covenants, conditions, and restrictions on the number of consecutive nights that they would be in use. If developed pursuant to the applicant’s “villas” model, however, these units would actually be detached structures with multiple bedrooms, similar to the residential developments oriented around golf courses, but with super-sized ground floor garages pushing living areas up to second and third floors. If counted as residences, ALUC’s density limitations in Airport Compatibility Zone C (one dwelling unit per five acres) would be exceeded, based on the amount of acreage originally included in the “Club.” The dwelling unit count for the Kohl Ranch Specific Plan as a whole (as established through Specific Plan No. 303 and its adopted amendments to date) would also be exceeded, absent reductions elsewhere within the boundaries of that Specific Plan. Additionally, at least 45 of the lots are located within the 60 dB(A) CNEL aircraft noise contour and some are within the 65 dB(A) CNEL aircraft noise contour, where new residential subdivisions are not permissible, pursuant to Policy N 7.3 of the Noise Element of the Riverside County General Plan. The recorded lots were originally established as nonresidential “members’ garage” lots.

**RECOMMENDATION:** Staff recommends a finding of INCONSISTENCY for the Specific Plan Amendment and Change of Zone for the reasons stated herein.

**PROJECT DESCRIPTION:** The Specific Plan Amendment proposes to amend the Kohl Ranch Specific Plan (most notably the Executive Summary, Project-Wide Planning Standards, and Land

Use, Planning & Development Standards sections) by defining and establishing a new use category, “racetrack recreational units,” as a permitted land use within Planning Area E-2 located in the portion of the Specific Plan northerly of Avenue 62 within the “Thermal Club.” The Change of Zone proposes to revise the Specific Plan zoning ordinance text in accordance with this proposal.

**PROJECT LOCATION:** The project site is located northerly of 62<sup>nd</sup> Avenue, easterly of Tyler Street, southerly of 60<sup>th</sup> Avenue, and westerly of Polk Street in the unincorporated community of Thermal, approximately 750 feet easterly and 2,700 feet southerly of the current southerly terminus of Runway 17-35 at Jacqueline Cochran Regional Airport. A portion of the project site is adjacent to the anticipated runway extension and Compatibility Zone A/Runway Protection Zone.

**LAND USE PLAN:** 2005 Jacqueline Cochran Regional Airport Land Use Compatibility Plan (last amended in 2006)

- a. Airport Influence Area: Jacqueline Cochran Regional Airport
- b. Land Use Policy: Airport Compatibility Zones C and D
- c. Noise Levels: 55-60, 60-65, and 65-70 CNEL

## **BACKGROUND:**

### Thermal Club “Founders’ Lots” ALUC Hearing History:

**December 29, 2009:** The first submittal was a proposal for a “substantial conformance” determination for changes to the text of the Kohl Ranch Specific Plan to allow for a club racetrack (ZAP1014TH09). ALUC’s primary concern at this stage was ensuring that the proposed racetrack would not be used for spectator sports or open to the public. The concept included provision for “founders’ lots and garages” along the edge of the track within a Planning Area designated for Heavy Industrial uses. At the time, it was envisioned that there would be 111 of these lots in Zone C and 20 in Zone D, for a total of 131 “garage” lots. The garage structures would provide club members with an indoor location for working on, and storing, the vehicles that would be utilizing the track and a venue for entertaining their day visitors. ALUC found the proposal consistent on March 11, 2010, subject to ten conditions. The letter included specific statements that the consistency finding was based on the representations by the landowner and representatives that the proposed automobile race track would be for the use of a club with limited membership, and that the approval of a finding of substantial conformance did not authorize the construction of any structures, which would require subsequent plot plans subject to ALUC review and approval.

**September 20, 2010:** The second submittal (ZAP1017TH10) included Plot Plan No. 24690 and Parcel Map No. 36293. This Commercial (“Schedule E”) Parcel Map provided for 254 “founders’ lots” for member garages – 152 in Zone C and 102 in Zone D. Staff’s main concern at the time was the observation tower/clubhouse (intensity and height). The application materials submitted by the applicant team specifically stated that “overnight occupancy [of the member garages] would be prohibited” (page 3 of 17 of “suggested” environmental analysis). The garages were to have two floors, with the upper floor allowing for an office, kitchen, and entertaining room. As the member

garages were understood to be nonresidential, single-acre intensities were calculated based on a “standard garage” plan that included 7,150 total square feet, of which not more than 4,320 square feet would be for office, kitchen, and entertainment. This was memorialized through Condition No. 12. In response to the applicant team’s concern that larger lots be allowed additional floor area, the applicability of Condition No. 12 was limited to lots not exceeding 7,540 square feet in net area. Condition No. 13 was added to address the overall concept and included a sub-section stating that no residential uses or overnight occupancy was permitted. ALUC found the proposal conditionally consistent (subject to FAA review of the observation tower/clubhouse) by a 5-2 vote (Commissioners Housman and Ballance dissenting) on October 14, 2010, with amendments to conditions 1, 8, 9, and 10, and addition of conditions 11 through 19. (FAA conditions 20 through 24 were subsequently added and included in the final letter.)

**March 5, 2012:** The third submittal (ZAP1019TH12) consisted of Substantial Conformance to Plot Plan No. 24690. It did not change the total number of garage lots, but modified the layout to provide for a more curvilinear street pattern in certain areas. ALUC found the proposal conditionally consistent (subject to FAA review of the observation tower/clubhouse) on April 12, 2012, with amendments to conditions 1, 4, 9, 11, 14, 15, and 16, and addition of new conditions 20 through 22. Previous FAA conditions 20 through 24 were deleted, and amended FAA conditions were subsequently added as numbered conditions 23 through 27. Among the amendments were the requirements that the contents of the notice language provided to potential purchasers and tenants also be contained in a legally recordable instrument to be recorded at time of map recordation or building permit issuance and that development on founders’ lots be reviewed for determination of whether FAA review for Obstruction Evaluation would be required.

**June 6, 2013:** The fourth submittal (ZAP1020TH13) consisted of Plot Plan No. 24690, Revised Permit No. 1 and Parcel Map No. 36293, Minor Change No. 1. The Parcel Map reconfigured and relocated founders’ lots within unrecorded portions of the parcel map. The number of lots was reduced to 250 – 146 in Zone C and 104 in Zone D. The Plot Plan revisions provided for an on-site irrigation reservoir. ALUC found the proposal consistent on September 12, 2013, amending Conditions 3a, 9, 12 through 17, renumbering Conditions 23 through 27 as 22 through 26, and adding Conditions 27 through 32.

The applicant team also proposed amending the conditions pertaining to the occupancy type of structures on the founders’ lots, raising concerns regarding the prohibition of overnight occupancy. ALUC’s staff report stated that “the fundamental problem here arises from the site’s Specific Plan and zoning, which are for industrial use. At this time, there is no definition available that would allow for residential occupancy of these structures.” It was pointed out that, with a Specific Plan amendment and ordinance amendment, the portion of the site in Zone D could potentially be used for residential purposes and that a portion of Zone C could also be utilized. The staff report also noted that “residential use and overnight occupancy would likely not be acceptable within units proposed to be located adjacent to Zone A.” ALUC’s determination retained the prohibition on overnight occupancy, as specified in Condition Nos. 13 and 15.

**October 28, 2014:** The fifth submittal relating to the founders' lots (ZAP1024TH14) consisted of Specific Plan No. 303, Amendment No. 3, Change of Zone Case No. 7852, and Tentative Tract Map No. 36851. The Specific Plan Amendment amended the land use designation of the founders' lot areas from Heavy Industrial to Mixed Use. The change of zone proposed to amend the allowable uses in these areas. The tract map proposed to subdivide 20 of the founders' lots for condominium purposes so as to allow each of the affected founders' lots to accommodate a duplex. The applicant team initially proposed that 151 of the private garages would allow overnight stays, along with development of 15 live/work units in the southwestern portion of the Club.

On January 8, 2015, ALUC issued a consistency determination for an amended proposal to allow overnight occupancy in the 120 units in Zone D and up to 39 units in Zone C. The limit of "39" was determined based on the total number of Thermal Club acres in Zone C (including area that had subsequently been approved for commercial/industrial purposes, such as a BMW facility), divided by 5 (since Zone C limits residential density to a maximum of one dwelling unit per five acres). The proposal for live/work units was deleted. ALUC Condition Nos. 12, 13, and 15 were amended to apply only to those lots not allowing overnight stays, and Condition No. 33 was added relating to building size and height on lots allowing overnight stays with net area not exceeding 7,540 square feet. (Condition No. 17 was also amended to reflect the deletion of the go-kart track.) The consistency determination included the allowance for duplexes on 19 of the lots, of which 16 are in Zone D and 3 are in Zone C, for a total of 38 condominium units.

**August 23, 2017:** Most recently, ALUC considered Plot Plan No. 24690, Revised Permit No. 2. The revision did not affect the founders' lots and consisted of proposals to develop a two-story, 7,040 square foot trackside garage with viewing deck, a member's club house facility, and six commercial hotel suite buildings. ALUC found the proposal consistent on October 12, 2017, by a 5-2 vote (Commissioners Butler and Stewart dissenting).

Current Status of Founders' Lots: Thirteen structures, including one duplex structure, have received final inspection approval. Eleven of the structures providing 12 units are in Zone D, and 2 are in Zone C. Building permits have been issued for an additional 8 structures (6 in Zone D and 2 in Zone C).

Residential Density: The Specific Plan Amendment proposes to allow for overnight stays within "racetrack recreational units" on most of the remaining "founders' lots" not previously approved for such use. The Amendment references 110 such units; however, staff's analysis indicates a potential total of 116, all located within Compatibility Zone C. (Of the 250 "founders' lots," 152 are located wholly or primarily within Compatibility Zone C. Three of these are duplex/condominium lots, bringing the potential total number of units in Zone C to 155. 155 minus the previously authorized 39 leaves 116 units on lots not previously approved for overnight occupancy.)

"Racetrack recreational units" would be a new category. Neither the Riverside County ALUCP nor the California Airport Land Use Planning Handbook provides for this category, so staff interprets and evaluates these as residential dwellings. However, the Thermal Club site has already been

allocated its allowable share of dwelling units in Zone C: 39 (see above).

Given the provisions of Table 2A limiting density in Zone C to one dwelling unit per five acres, not more than 39 dwelling units would be permissible within that Compatibility Zone, as determined at ALUC's January 8, 2015 public hearing relating to Specific Plan No. 303, Amendment No. 3. Any additional density would exceed the allowance for dwelling units within Compatibility Zone C. If the proposed residential use were permitted on all of the founders' lots, the residential density in Zone C would climb to 0.79 dwelling units per acre (one dwelling unit per 1.26 acres). Therefore, staff finds this proposal to be inconsistent with the Jacqueline Cochran ALUCP.

The applicant team is suggesting that the structures not be considered to be residences. The applicant proposes a residential equivalent of an "intermittent use." The Club would be responsible for enforcing limits on the number of overnight stays. Specifically, the applicant team is proposing to amend covenants, conditions, and restrictions so that use of the "Racetrack Recreational Units" on 110 of the lots in Zone C be limited to a maximum of 45 consecutive nights each, with owners/visitors/occupants who overstay subject to loss of track rights and club privileges. (Assuming 3.2 persons per unit, that would mean a cumulative total of 15,840 person-nights in any 45-day period, if all of these units were occupied simultaneously.) The amendment would also provide for submittal of a quarterly report to ALUC providing a summary of overnight stays during the reporting period. However, ALUC has no ability to monitor or enforce compliance with the consecutive overnight stay provisions. Neither the Riverside County ALUCP nor the California Airport Land Use Planning Handbook authorize ALUC to restrict uses inside a residence after it has been constructed.

Zoning Ordinance: The applicant proposes to amend the zoning ordinance text applicable to Planning Area E-2 to add "racetrack recreational units" as a permitted use, to limit the number of such units to 110, and to specify permitted uses for each floor of such units. The text would also be amended to delete hospitals, libraries, and religious institutions from the list of land uses permissible with plot plan approval in this Planning Area.

Prohibited Uses: Children's schools, hospitals, nursing homes, libraries, and day care centers are listed as prohibited uses within Compatibility Zone C, pursuant to Table 2A of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan. None of these uses are proposed within this project.

Noise: The site is located within the area subject to average aircraft noise levels ranging from 55-60 CNEL (Community Noise Equivalent Level) on the east side to 65-70 CNEL on the west. As the garages initially proposed for the "Founders' lots" were non-residential/non-sensitive uses, no special measures to mitigate aircraft-generated noise were required. With Amendment No. 3 allowing for dwelling units/overnight stays (120 units in Zone D and 39 units in Zone C), aircraft noise became a relevant concern. However, the units authorized for overnight stays were located within areas subject to aircraft noise below 60 CNEL. Typical construction methods would typically reduce noise levels by 20 dB(A), thus reducing interior noise levels from aircraft from up to 60



CNEL to 40 CNEL, which is generally acceptable for residential land uses. Additionally, given the nature of the project with adjacent track area and vehicle noise during the day as well as large garage areas within each member's private garage that would allow for individuals working on their vehicles 24 hours a day, residential units would be subject to high noise levels from on-site activities.

Currently, overnight stays are not permitted within Lots 90 through 201 (the northerly portion of lots within Compatibility Zone C). The noise contours were mapped prior to the recordation of these lots, but, based on the contours depicted on Figure 3.1-14 of the Specific Plan, approximately 25 of these lots would be subject to average noise levels exceeding 65 CNEL, and an additional 50 lots to average noise levels between 60 CNEL and 65 CNEL. Policy N 7.3 in the Noise Element of the Riverside County General Plan prohibits "new residential land uses, except construction of a single-family dwelling on a legal residential lot of record, within the current 60dB(A) CNEL contours of any currently operating public-use, or military airports." Parcel Map No. 36293 was approved as a Schedule "E" (commercial/industrial) parcel map, not as a tract map. The approval and recordation of a tract map would have been the process required to establish a residential subdivision. A residential subdivision that included Lots 90 through 201 (specifically, the 75 +/- lots subject to average aircraft noise levels exceeding 60 CNEL) would violate/be inconsistent with the above-referenced Policy 7.3 of the Noise Element. Uses such as hotels and motels that provide for overnight stays are not subject to this noise-based constraint on development. However, a hotel suite is different than an individually owned freestanding structure

PART 77: The issue being addressed today is whether to allow overnight stays in structures to be built in Planning Area E-2. This Planning Area and the lots included therein are closer to the runway than the areas where units have been constructed to date. Limits on the heights of structures would apply. Most of the founders' lots do not have specific buildings proposed at this time; however, based on the "Standard Garage" plan and the pad elevations, numerous buildings would require review by FAA. Existing ALUC Condition No. 20 requires review by FAA for individual buildings at time of building permit application based on the specific building proposed at that time and the reference table provided in 2012.

Open Area: Compatibility Zones B1, C and D require that 30%, 20%, and 10% of area within major projects (10 acres or larger) be set aside as open land that could potentially serve as emergency landing areas. No changes to the previously designated open areas are proposed through this amendment.

In the course of review of Amendment No. 3, ALUC staff made the following determinations as to open area: "All of the 110.0 acres of the project located within Zone A is set aside consistent with the open space criteria for Zone A. 38.46 acres of the 69.9 acres (55%) of the project area located within Compatibility Zone B1 qualifies as open area. 71.72 acres of the 155 acres (46%) of the project area located within Compatibility Zone C qualifies as open area. 13.47 of the 51.6 acres (26%) of the project area located within Compatibility Zone D qualifies as open area. In total, the project proposes approximately 233.6 acres of open area, which is 64.97 acres greater than the

168.63 minimum acres required.” The open area is to be maintained as ALUCP-compliant open area (meeting the requirements of Section 4.2.4(a) of the Countywide Policies).

Thermal Club Website: The Thermal Club website, Villas section, states that villas “range in size from 8,000 to 15,000 square feet” and include “three-to-five bedrooms.” While the 159 dwelling units authorized through Amendment No. 3 are not necessarily limited in size if built on larger lots, and are not limited in number of bedrooms, these levels of intensity would not be consistent for Lots 90 through 201. In fact, ALUC Condition No. 12, as amended by ZAP1024TH14, requires ALUC review for any structures exceeding 7,150 square feet in floor area or more than 4,320 square feet of uses other than garage, storage, and warehousing uses on lots with net areas not exceeding 7,540 square feet that do not allow overnight stays. This requirement was based on the evaluation of these structures for nonresidential intensity. The advertised “Villas” exceed the size currently allowed on the standard-sized lots. Amending the Specific Plan and zoning ordinance to allow overnight stays on these lots would essentially render Conditions 12 and 13 of ZAP1024TH14 inapplicable, since, as written, they currently apply only to those lots not allowing overnight stays, unless the conditions are amended so as to continue to apply to the lots. Condition No. 33 applies to lots with net areas not exceeding 7,540 square feet that do allow overnight stays, and specifically references Planning Areas E-5 through E-8. If this proposed Specific Plan Amendment were approved, Condition No. 33 would need to be amended to include Planning Area E-2.

Since this proposal is being recommended for a finding of inconsistency, staff is not proposing any amendments to ALUC’s recommended conditions as issued following the January 8, 2015 hearing on Specific Plan No. 303, Amendment No. 3.

However, in the event that ALUC or the County render a decision allowing overnight stays on all or almost all of the founders’ lots, consideration should be given to incorporate some or all of the provisions of Condition No. 13 relating to floor area ratio, building height, and parking into a revised Condition No. 33.

## **CONDITIONS:**

**Staff is not proposing any new conditions for the Specific Plan Amendment.** The following conditions are from the latest ALUC review for the overall Thermal Motorsports Project (ZAP1024TH14 for SP00303A3), provided as a reminder of the existing conditions of approval pertaining to the Plot Plan. (These conditions pertain to the portion of the Specific Plan known as the Thermal Club and the proximate open areas thereof. Conditions relating to other areas remain as specified in the ALUC letters addressing Specific Plan No. 303, Amended No. 2 and Specific Plan No. 303, Amended No. 1.)

1. Prior to the issuance of building permits, the landowner shall convey an avigation easement to Jacqueline Cochran Regional Airport, which shall be recorded, or shall provide evidence that such an easement covering the property has already been recorded. Copies of the avigation easement, upon recordation, shall be forwarded to the Riverside County Planning

Department and to the Riverside County Airport Land Use Commission.

[This condition shall be considered as satisfied.]

2. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky, and shall comply with Riverside County Ordinance No. 655, as applicable. Outdoor lighting plans, if any, shall be transmitted to Riverside County Economic Development Agency – Aviation Division personnel and to the Jacqueline Cochran Regional Airport for review and comment. (Failure to comment within thirty days shall be considered to constitute acceptability on the part of the airport manager.)
3. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations, or any type of strobe light, toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator. (Amended 2013)
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, livestock operations, production of cereal grains, sunflower, and row crops, artificial marshes, wastewater management facilities, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, incinerators, and landfills.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Children’s schools, day care centers, libraries, hospitals, nursing homes, places of worship, highly noise-sensitive outdoor nonresidential uses, and aboveground bulk storage of 6,000 gallons or more of hazardous or flammable materials.
4. The attached notice shall be provided to all potential purchasers and tenants and the contents of such notice language shall also be contained in a legally recordable instrument to be recorded at time of map recordation or building permit issuance.

5. Any detention or retention basin shall be designed so as to provide a maximum 48-hour detention period for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the retention basin that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
6. Noise attenuation measures shall be incorporated into the design of office areas of structures, as necessary to ensure interior noise levels from aircraft operations are at or below 45 CNEL.
7. No portion of any roadway or track shall be located within the Runway Protection Zone.
8. No use of the automobile racetrack for the purpose of spectator sports, in which guests pay for admission to an event or series of events, or to which the general public is invited, is included in this determination of consistency.
9. Development of the area addressed through Plot Plan No. 24690 Revised Permit No. 1 shall comply with all nonresidential intensity criteria and open area requirements of the applicable airport compatibility zones.
10. All structures shall maintain a minimum perpendicular distance of 750 feet from any point along the centerline of Runway 17-35 of Jacqueline Cochran Regional Airport, as the runway is depicted on the Airport's Master Plan (including any point on the centerline of the runway as extended to the southerly boundary of Airport Compatibility Zone A).
11. Occupancy of the 3<sup>rd</sup> floor of the Control Tower shall be limited to track control officials only or their designees.
12. Prior to building permit issuance on any of the Founders' Lots **not allowing overnight stays within Planning Area E-2 and** with a net area of 7,540 square feet or less, County Plan Check officials shall verify that either: (1) the proposed buildings does not exceed the "Standard Garage" plan or (2) the larger building has been submitted to the Riverside County Airport Land Use Commission staff and determined to be consistent. The "Standard Garage" shall be defined as having a total square footage not exceeding 7,150 square feet, with office, entertainment, and kitchen areas, as applicable not exceeding 4,320 square feet, and the remainder of the building devoted to storage, garage, and warehousing uses, and a height not exceeding two stories or 42 feet. Any building on such lots proposing either (1) a total square footage exceeding 7,150 square feet or (2) more than 4,320 square feet of uses other than storage, garage, and warehousing uses, or with a height exceeding two stories or 42 feet, shall be submitted to the Riverside County Airport Land Use Commission for review.

(Amended by ZAP1020TH13 and ZAP1024TH14)

13. Development on Founders' Lots **not allowing overnight stays within Planning Area E-2** shall comply with the following standards: (1) the floor area ratio shall not exceed 0.95; (2) lot coverage shall not exceed 0.5; (3) the proportion of the building allocated to uses other than storage, garage, and warehousing uses shall not exceed 0.6; (4) no uses more intense than office uses and no assembly uses are permitted; (5) no residential uses or overnight occupancy (occupancy between the hours of 10:00 P.M. and 6:00 A.M. – between 2200 hours and 600 hours military time) is permitted; (6) the building does not exceed 42 feet in height; (7) no parking spaces are provided outside of the garage; and (8) garages contain a minimum space for two automobiles. If any of these criteria are not met, the building shall be submitted to the Riverside County Airport Land Use Commission for review.

(Amended by ZAP1020TH13 and ZAP1024TH14)

14. The following special occupancy load restrictions shall be posted:
- a) The maximum number of persons permitted in the tower building at any given time shall not exceed one hundred fifty (150) persons.
  - b) The maximum number of persons permitted in the members' storage garage in the village area at any given time shall not exceed seventy-five (75) persons.
  - c) The maximum number of persons permitted in the tuning shop building at any given time shall not exceed one hundred fifty (150) persons.

(Amended by ZAP1020TH13)

15. A notice to potential purchasers of lots **not allowing overnight stays within Planning Area E-2**, indicating that no residential uses or overnight occupancy (between 10:00 P.M. and 6:00 A.M. – between 2200 and 600 hours military time) shall be permitted, shall be provided in the form of a legally recordable instrument to ALUC staff for review and approval regarding content of the notice. Said instrument shall be recorded at the time of map recordation for each unit of Parcel Map No. 36293. Prior to sale of any individual lot, this notice shall be provided to potential purchasers. This restriction shall also be included within CC&Rs. This restriction does not apply to the nonresidential use of the tuning shop and members' storage garage in the village area for purposes of vehicle repair and maintenance during those hours, under the supervision of Club officials.

(Amended ZAP1020TH13 and ZAP1024TH14)

16. No trees, light poles, utility poles, or any other object greater than four feet in height and thicker than four inches shall be allowed within designated open areas.

(Amended by ZAP1020TH13)

17. Per the applicant's comment, racing on the track shall be limited to the hours of 7:00 A.M. to 7:00 P.M.

(Amended by ZAP1020TH13 and ZAP1024TH14)

18. No pole affixed lighting shall be allowed on interior private streets.
19. The control tower shall be limited to a maximum 3 above ground habitable floors.
20. Development on Founders' Lots shall be reviewed for determination of whether FAA review is required for Obstruction Evaluation. The Exhibit titled Buildings Summary Table and dated March 27, 2012 shall be used as a guide for determining whether a building is required to be reviewed based on the pad elevation, building height, distance to the ultimate end of the runway, elevation of the ultimate end of the runway, and a relevant slope ratio of 1:100. ALUC staff shall be consulted if there is any issue with this determination at time of building permit application. If FAA review is deemed to be required, the development shall comply with any subsequent determination and conditions from the FAA.
21. Any future revisions to the Plot Plan or any specific proposal for grading or pad elevations for Phase II as identified on the Substantial Conformance Exhibit for Plot Plan No. 24690 dated 3/20/12 shall be transmitted to ALUC staff for review to determine whether submittal to ALUC is required. This review is intended to confirm any changes in intensities proposed and to determine whether FAA review for Obstruction Evaluation may be required.
22. The Federal Aviation Administration has conducted an aeronautical study of the control tower building (Aeronautical Study Nos. 2012-AWP-2704-OE through 2012-AWP-2707-OE) and has determined that neither marking nor lighting of the structure is necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 K Change 2 and shall be maintained in accordance therewith for the life of the project.
23. The maximum height of the structure, including all roof-mounted appurtenances (if any) shall not exceed 61 feet above ground level, and the maximum elevation at the top of the structure shall not exceed 82 feet below mean sea level.
24. The specific coordinates, height, and top point elevation of the control tower structure shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in building height or elevation shall not require further review by the Airport Land Use Commission.
25. Temporary construction equipment used during actual construction of the structural

improvements shall not exceed the height of the building (61 feet above ground level), unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.

26. Within five (5) days after construction of the control tower reaches its greatest height, FAA Form 7460-2, Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and submitted to the Federal Aviation Administration Southwest Regional Office Obstruction Evaluation Group, 2601 Meacham Boulevard, Fort Worth TX 76137. This requirement is also applicable in the event the project is abandoned.
27. The irrigation reservoir shall be completely covered at all times from top, sides, and bottom so as to prevent access by birds and other wildlife. The cover shall consist of 1" x 1" UV-protected polypropylene mesh secured at ground level around the edges and suspended four feet above the edge elevation, on steel cables spaced not greater than 30 feet apart, as depicted on the attached exhibits. The suspension design is intended to allow for the sagging of the netting material without touching the surface of the waters, so that the material stays dry. The cables and netting material shall be maintained in operable condition (no gaps or tears) throughout the life of the permit, as long as the reservoir holds water or other liquid.

(Added by ZAP1020TH13)

28. Prior to issuance of a letter of consistency for this project, the applicant team shall submit to the Riverside County Airport Land Use Commission a letter from a qualified airport wildlife biologist (as specified in FAA Advisory Circular 150/5200-36) stating that he/she has reviewed the proposed reservoir design and finds that the proposed measures will be effective in mitigating wildlife attraction to the facility.

(Added by ZAP1020TH13)

29. In the event that any incidence of wildlife hazard affecting the safety of air navigation occurs as a result of the presence of the irrigation reservoir on-site, upon notification to the airport operator (currently the Riverside County Economic Development Agency) of an incidence, the airport operator shall notify Thermal Operating Company, LLC (or its successor(s)-in-interest) (hereafter referred to as "owner") in writing. Within 15 days of written notice, the owner shall be required to promptly take all measures necessary to eliminate such wildlife hazard, including, if necessary, the emptying of the reservoir and replacement of the netting material. An "incidence" includes any situation that results in an accident, incident, "near-miss," or specific safety complaint regarding an in-flight experience to the airport operator or to federal, state, or county authorities responsible for the safety of air navigation. The owner shall work with the airport operator to prevent recurrence of the incidence. Suggested measures may include providing for scheduled joint inspections of the reservoir by representatives of the owner and the airport to assure that the cables and netting material continue to prevent access to the waters. For each such incidence made known to the owner,

the necessary remediation shall only be considered to have been fulfilled when the airport operator states in writing that the situation has been remediated to the airport owner's satisfaction.

(Added by ZAP1020TH13)

30. The covenants, conditions, and restrictions established for this project shall specify that any splash pools or other water features associated with individual member garage units shall be equipped with electronic covers. The water shall not be allowed to stagnate and shall be completely covered at all times when the individual member garage unit is not in immediate use.

(Added by ZAP1020TH13)

31. **Condition No. 31 was satisfied, but has since been rendered inapplicable as a result of the adoption of Specific Plan No. 303, Amendment No. 3.**

~~This finding of consistency does not pertain to the following notes listed on the PP24690R1 exhibit plotted on July 22, 2013:~~

~~(a) — Note that Occupancy Type for the Members' Private Garages is R/S2.~~

~~(b) — Note that overnight stays are permitted in the D zone.~~

~~The project is located within an area designated for industrial use within Specific Plan No. 303. Prior to issuance of a letter of consistency for this project, the applicant team shall submit an amended exhibit that excises these references.~~

32. Prior to issuance of building permits for garage units on Lots 156 through 201, a block wall shall be constructed along the property line separating the easterly boundary of the airport property and the private street providing access to these properties.

(Added by ZAP1020TH13)

33. Prior to building permit issuance on any of the Founders' Lots allowing overnight stays within Planning Areas E-5, E-6, E-7, E-8 and with a net area of 7,540 square feet or less, County Plan Check officials shall verify that either: (1) the proposed building does not exceed the "Standard Unit" plan or (2) the larger building has been submitted to the Riverside County Airport Land Use Commission staff and determined to be consistent. The "Standard Unit" has been defined as having a total square footage not exceeding 7,150 square feet and a height not exceeding two stories or 42 feet. Any building on such lots proposing either (1) a total square footage exceeding 7,150 square feet or (2) more than a height exceeding two stories or 42 feet, shall be submitted to the Riverside County Airport Land Use Commission for review.

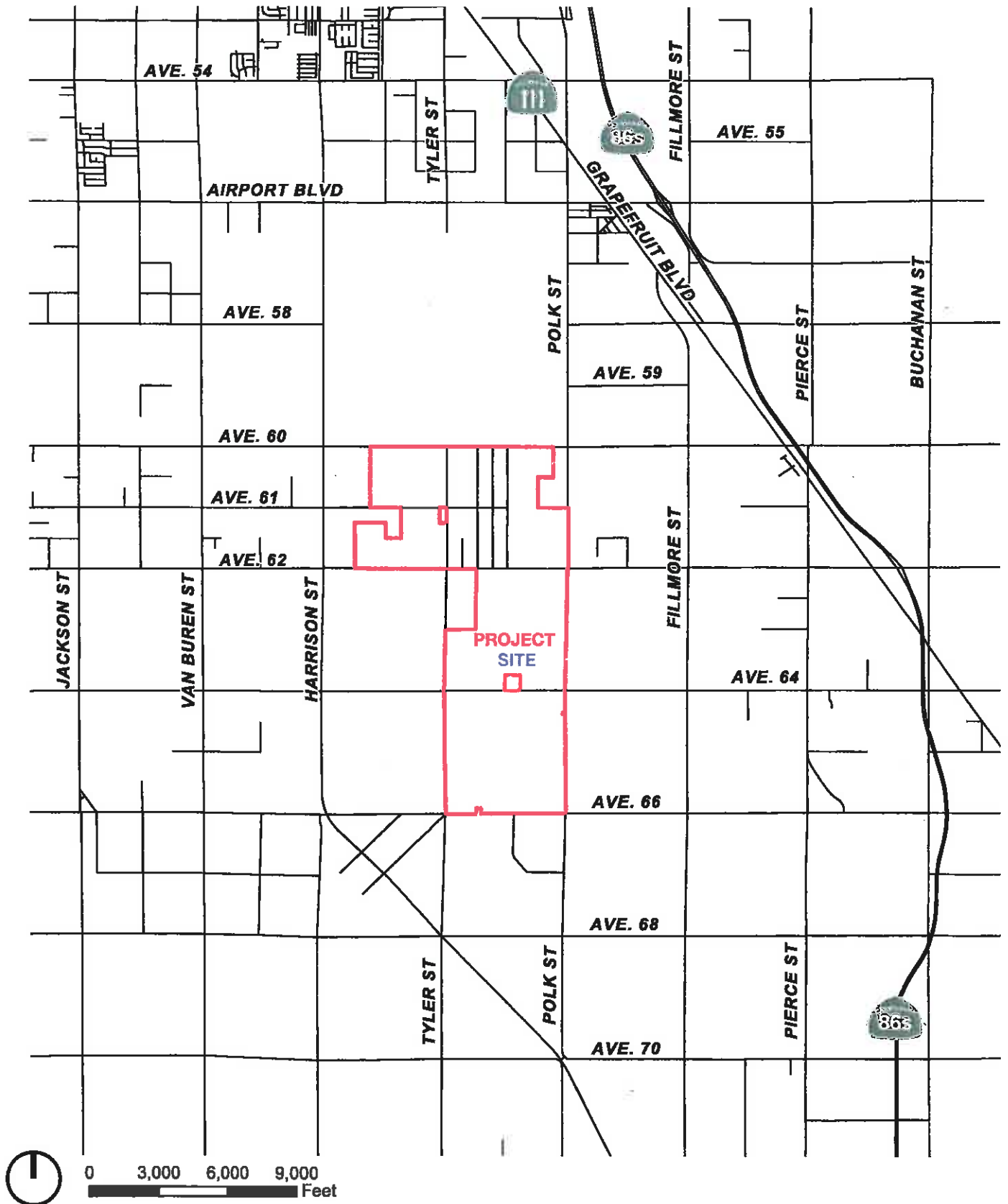
(Added by ZAP1024TH14)



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# NOTICE OF AIRPORT IN VICINITY

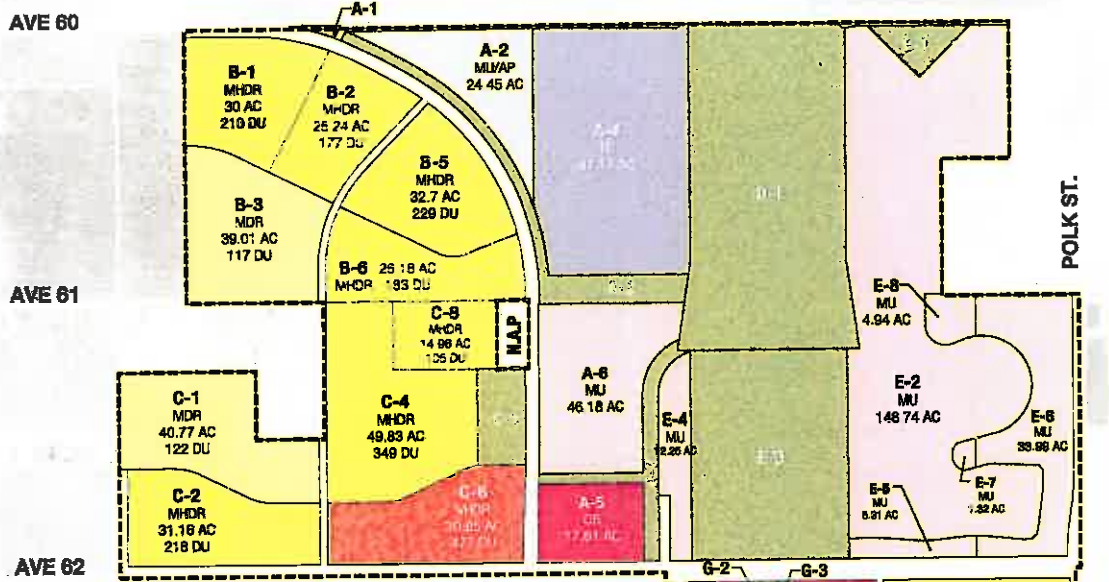
This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)







Normanton St



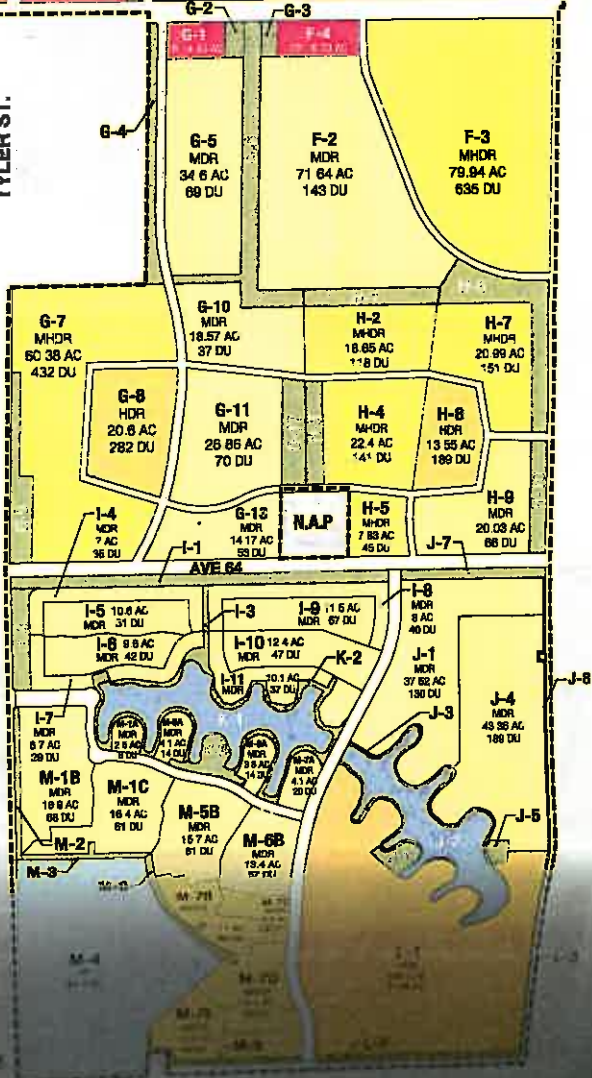
LEGEND

OPEN SPACE PLANNING AREAS

Area	Land Use	Acres
A-1	OS	0.53
A-3	OS	10.77
A-7	OS	9.97
C-6	OS	9.97
D-1	OS	110.88
E-1	OS	5.61
E-3	OS	69.33
G-2	OS	1.44
G-8	OS	1.19
G-4	OS	5.8
G-6	OS	13.65
G-9	OS	6.17
G-12	OS	5.35
H-1	OS	5.12
H-8	OS	3.79
H-8	OS	11.43
H-10	OS	2.98
I-1	OS	9.6
I-2	OS	3.9
I-9	OS	0.5
J-2	OS LAKE	22
J-3	OS LAKE	8.8
J-5	OS	1
J-6	OS	2.2
J-7	OS	3.3
J-8	OS	1.3
K-1	OS LAKE	22.4
K-2	OS LAKE	8.8
K-9	OS	3.2
L-2	OS	4.1
L-8	OS	1.1
M-2	OS	1.9
M-3	OS	1.8
M-8	OS	2.9
M-9	OS	2.7
TOTAL		576.04

- PROJECT AREA BOUNDARY
- YELLOW MEDIUM DENSITY RESIDENTIAL (MDR)
- ORANGE MEDIUM HIGH DENSITY RESIDENTIAL (MHDR)
- RED HIGH DENSITY RESIDENTIAL (HDR)
- RED VERY HIGH DENSITY RESIDENTIAL (VHDR)
- PINK COMMERCIAL RETAIL (CR)
- PURPLE MIXED USE (MU)
- PURPLE MIXED USE/AIR PARK (MU/AP)
- GREEN HEAVY INDUSTRIAL (HI)
- GREEN PUBLIC FACILITY/SCHOOL (PF)
- GREEN OPEN SPACE (OS)
- BLUE OPEN SPACE/LAKE (OS-LAKE)
- WHITE RIGHT OF WAY

TYLER ST.

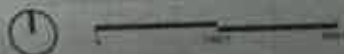


LAND USE

MEDIUM DENSITY RESIDENTIAL (MDR)  
 MEDIUM HIGH DENSITY RESIDENTIAL (MHDR)  
 HIGH DENSITY RESIDENTIAL (HDR)  
 VERY HIGH DENSITY RESIDENTIAL (VHDR)

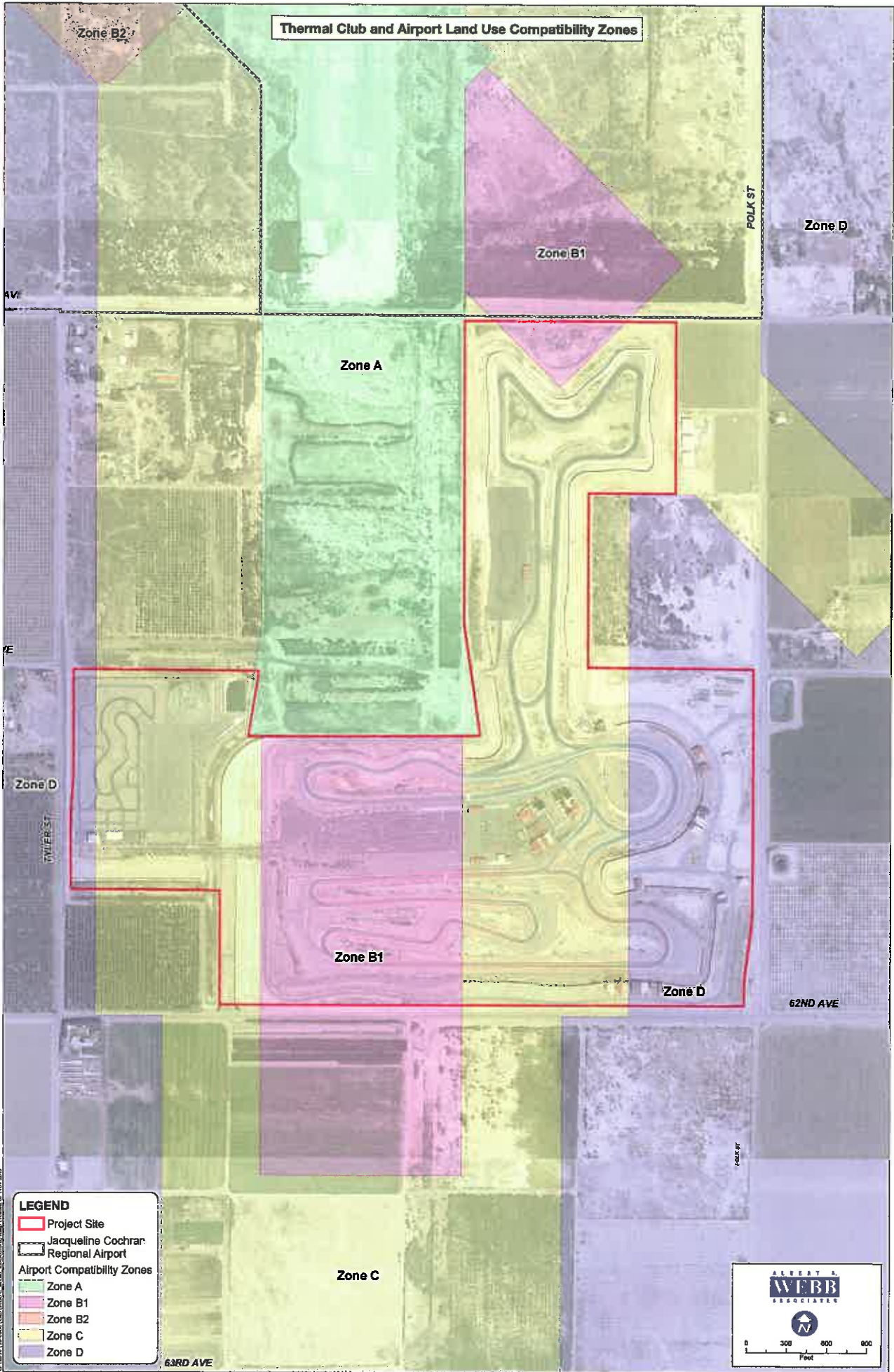
DENSITY RANGES

10-50 DU/AC  
 15-75 DU/AC  
 20-100 DU/AC  
 25-150 DU/AC



THE KOHL RANCH

Thermal Club and Airport Land Use Compatibility Zones



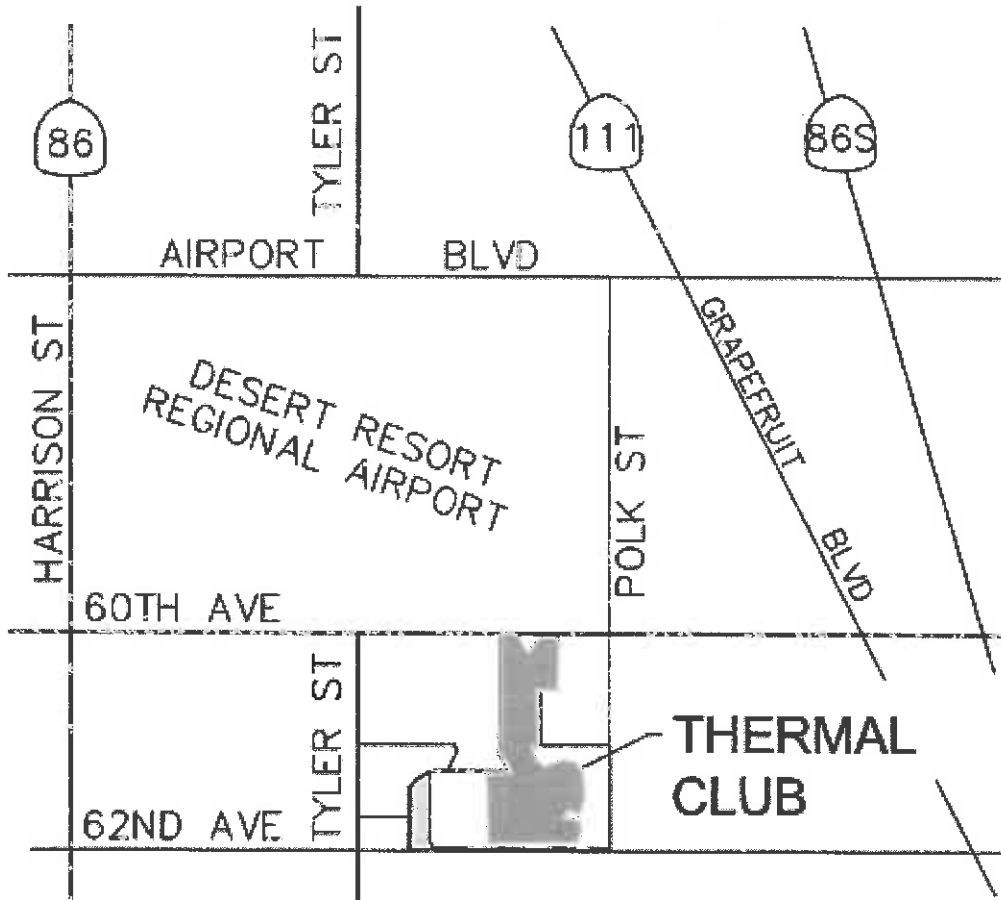
**LEGEND**

- Project Site
- Jacqueline Cochran Regional Airport
- Airport Compatibility Zones
  - Zone A
  - Zone B1
  - Zone B2
  - Zone C
  - Zone D

ALBERT A. WEBB ASSOCIATES

0 300 600 900  
Foot

© 2017, W&A (ES/ALIC) aerial, 24. Ward, Map created by Nov 2017  
Source: Riverview Co. GIS, 2017; Imagery: TerraServer, Apr 2017.

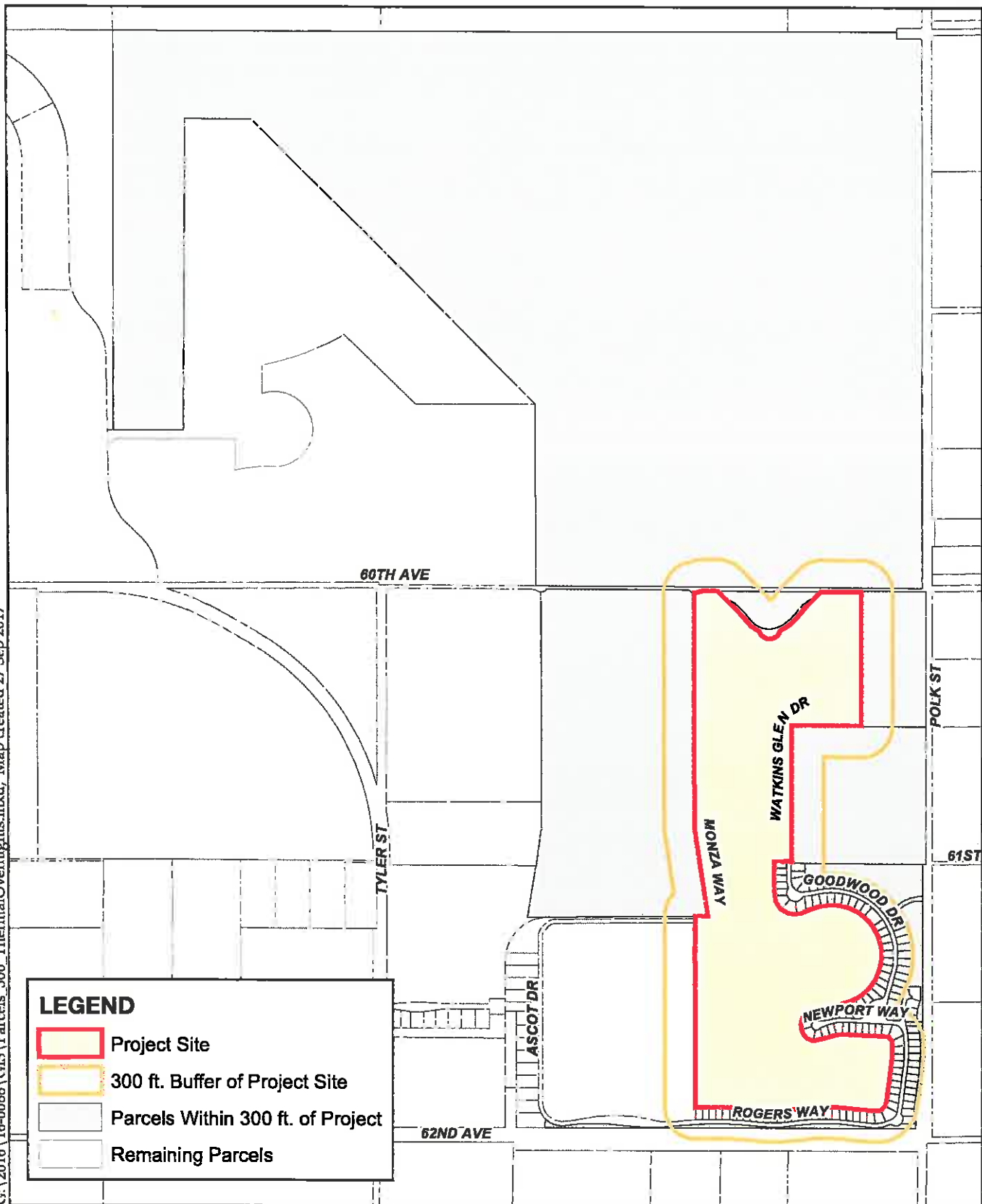


VICINITY MAP

NTS



G:\2016\16-0066\GIS\Parcels\_300\_ThermalOvernights.mxd; Map created 27 Sep 2017

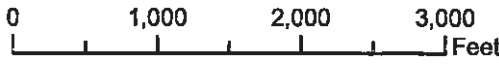


**LEGEND**

- Project Site
- 300 ft. Buffer of Project Site
- Parcels Within 300 ft. of Project
- Remaining Parcels

Source: Riverside Co. GIS, July 2017.

### Parcels Within 300 ft. of Project Thermal Overnight Stays



EXCERPT: SUBSTANTIVE AMENDMENTS TO TEXT OF SPECIFIC PLAN  
(ADDED TEXT UNDERLINED, DELETED TEXT IN ~~STRIKETHROUGH~~)

### 1.2.3 Proposed Changes to Specific Plan No. 303

The Kohl Ranch Specific Plan No. 303, Amendment 3-4 (SPA3SPA4) is a result of a change in land use allowances located in the northeasterly area of the project site (specifically Planning Area E-2). SPA3 SPA4 includes the addition of outdoor film studios, and racing facility related residential racetrack recreational uses and live/work units as allowable land uses under the new Mixed Use land use designation for Planning Area E-2, update of the zoning ordinance, adjustments to the design guidelines for the Mixed Use land use designation, and a more detailed specific plan implementation and administrative processing discussion. Existing planning area E-2 has been divided into five new planning areas: E-2, E-5, E-6, E-7, and E-8. Existing planning areas A-8 and E-4 will be combined to create one planning area (E-4). Planning areas A-6, E-2, E-4, E-5, E-6, E-7, and E-8 will be designated Mixed Use to identify and describe land use restrictions and conditions relative to ALUC Safety zones, and identify the special development needs related to a motorsports racing park. An adjustment has been made to the zoning ordinance through a change of zone (CZ07852 CZXXXX), and is reflected in this Specific Plan document to define and include horizontal and vertical mixed use development and outdoor film studio uses to the Mixed Use designation.

Target densities have been adjusted for planning areas F-2, G-5, G-10, G-11, H-2, and H-4 to allow for mixed use residential units in planning areas E-5, E-6, E-7, and E-8 to as not to exceed the maximum unit count of 7,171.

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### 1.2.4 Project Overview

The Kohl Ranch Specific Plan Amendment 3-4 consists of a balanced array of land uses including residential, business, commercial, industrial, open space/recreation, and public facilities as depicted in Figure 1-4, Land Use Plan and 1-5). Both living and working opportunities will be available within the project. The residential portion includes target of 7,162 dwelling units but with a maximum not exceed 7,171 dwelling units distributed among four different density classifications on 1,140.29 acres, with a gross residential density of 4.03 dwelling units per acre<sup>3</sup>. Although the maximum density and the total number of dwelling units within the high density and very high density residential designations can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code. Approximately 375.94 acres of open space provide for passive and active recreation, including trails and parks. The plan also allows for the development of large-scale recreational uses such as a golf course and a motor sports race track, which are identified as allowable land uses.

<sup>3</sup> The overall gross density for residential areas was determined by dividing the total number of dwelling units by the acreage devoted to residential land uses, open space, public facilities, and local residential streets. The gross density—if determined by dividing the total number of dwelling units by the residential acreage—is 6.28. This density is reduced to 3.31, if the total site acreage is used.

**Table 1-A, Land Use Diagram Statistical Summary**

Land Use	Acreage	Percent of Total	Percent of Developable Area <sup>1</sup>	Target Dwelling Units
<b>RESIDENTIAL</b>				
Medium Density Residential (MDR)	501.33	23.2	32.8	1,652
Medium High Density Residential (MHDR)	467.76	21.6	30.6	3,245
High Density Residential (HDR)	140.35	6.5	9.2	1,629
Very High Density Residential (VHDR)	30.85	1.4	2.0	477
<b>Total Residential</b>	<b>1,140.29</b>	<b>52.7</b>	<b>74.7</b>	<b>7,003</b>
<b>INDUSTRIAL</b>				
Heavy Industrial (HI)	81.17	3.8	5.3	
<b>Total Industrial</b>	<b>81.17</b>	<b>3.8</b>	<b>5.3</b>	
<b>BUSINESS</b>				
Mixed Use/Air Park (MU/AP)	24.45	1.1	1.6	
Mixed Use	252.73	11.7	16.6	159 <sup>3</sup>
<b>Total Business</b>	<b>277.18</b>	<b>12.8</b>	<b>18.2</b>	<b>159<sup>3</sup></b>
<b>COMMERCIAL</b>				
Commercial-Retail (CR)	28.27	1.3	1.9	
<b>Total Commercial</b>	<b>28.27</b>	<b>1.3</b>	<b>1.9</b>	
<b>OTHER</b>				
Open Space (OS)	375.94	17.4		
Public Facilities/Schools (PF)	84.30	3.9		
Right-of-Way (ROW)	175.50	8.1		
<b>Total Other</b>	<b>641.44</b>	<b>29.6</b>		
<b>TOTAL</b>	<b>2,162.65</b>	<b>100%</b>	<b>100%</b>	<b>7,162</b>

1. Assumes total of 1,526.91 acres of developable land. This does not include land uses in "other" category.
2. The ROW acreage includes local streets, which are not depicted on the Land Use Plan.
3. This number represents a maximum rather than a target.

**Table 1-B, Land Use Summary by Planning Area**

Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
A-1	Open Space (OS)		0.63	
A-2	Mixed Use/Air Park (MU/AP)		24.45	
A-3	Open Space (OS)		10.77	
A-4	Heavy Industrial (HI)		81.17	
A-5	Commercial Retail (CR)		17.61	
A-6	Mixed Use (MU)		46.18	
A-7	Open Space (OS)		9.97	
A-8	Heavy Industrial (HI)		6.55	
B-1	Medium High Density Residential (MHDR)	210	30	5.0-8.0
B-2	Medium High Density Residential (MHDR)	177	25.24	5.0-8.0
B-3	Medium Density Residential (MDR)	117	39.01	2.0-5.0
B-5	Medium High Density Residential (MHDR)	229	32.70	5.0-8.0
B-6	Medium High Density Residential (MHDR)	183	26.18	5.0-8.0
C-1	Medium Density Residential (MDR)	122	40.77	5.0-8.0
C-2	Medium High Density Residential (MHDR)	218	31.16	5.0-8.0
C-4	Medium High Density Residential (MHDR)	349	49.83	5.0-8.0
C-5	Open Space (OS)		9.97	
C-6	Very High Density Residential (VHDR)	477	30.85	14.0-20.0
C-8	Medium High Density Residential (RM)	105	14.96	5.0-8.0
D-1	Open Space (OS)		110.66	
E-1	Open Space (OS)		5.61	
E-2 <sup>1</sup>	Mixed Use (MU)		148.74	
E-3	Open Space (OS)		69.33	
E-4	Mixed Use (MU)		12.25	
E-5	Mixed Use (MU)	19 <sup>1</sup>	5.31	
E-6	Mixed Use (MU)	120 <sup>1</sup>	33.99	
E-7	Mixed Use (MU)	5 <sup>1</sup>	1.32	
E-8	Mixed Use (MU)	15 <sup>1</sup>	4.94	
F-2	Medium Density Residential (MDR)	158	71.64	2.0-5.0



Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
M-7C	Medium High Density Residential (MDR)	32	5.9	5.0-8.0
M-7D	Medium High Density Residential (MDR)	82	15.1	5.0-8.0
M-7E	Medium High Density Residential (MHDR)	82	15.5	5.0-8.0
M-8	Open Space (OS)		2.9	
M-9	Open Space (OS)		2.7	
Right-of-Way			175.50	
<b>TOTAL</b>		<b>7,161</b>	<b>2,162.65</b>	

Note: Planning Areas B-4, B-7, C-3, and C-7 have been intentionally left out.

- 1. This number represents a maximum, rather than a target.
- 2. This planning area allows for 110 racetrack recreational units

The business, commercial, and industrial land use categories will comprise 277, 28, and 81 acres, respectively. Commercial areas will serve the Kohl Ranch project as well as neighboring communities. Business and industrial uses will be oriented toward the Jacqueline Cochran Regional Airport as well as larger regional markets, and are intended to provide employment opportunities to project area residents. The land uses proposed for the Kohl Ranch Specific Plan are described in **Figure 1-5, Land Use Plan** and are briefly summarized in **Table 1-A**, above.

Table 3.1-A, Land Use Plan Statistical Summary

Land Use	Acreeage	Percent of Total	Percent of Developable Area <sup>1</sup>	Target Dwelling Units
<b>RESIDENTIAL</b>				
Medium Density Residential (MDR)	501.33	23.2	32.8	1,652
Medium High Density Residential (MHDR)	467.76	21.6	30.6	3,245
High Density Residential (HDR)	140.35	6.5	9.2	1,629
Very High Density Residential (VHDR)	30.85	1.4	2.0	477
<b>Total Residential</b>	<b>1,140.29</b>	<b>52.7</b>	<b>74.7</b>	<b>7,003</b>
<b>INDUSTRIAL</b>				
Heavy Industrial (HI)	81.17	3.8	5.3	
<b>Total Industrial</b>	<b>81.17</b>	<b>3.8</b>	<b>5.3</b>	
<b>BUSINESS</b>				
Mixed Use/Air Park (MU/AP)	24.45	1.1	1.6	
Mixed Use	252.73	11.7	16.6	159 <sup>3</sup>
<b>Total Business</b>	<b>277.18</b>	<b>12.8</b>	<b>18.2</b>	<b>159<sup>3</sup></b>
<b>COMMERCIAL</b>				
Commercial Retail (CR)	28.27	1.3	1.9	
<b>Total Commercial</b>	<b>28.27</b>	<b>1.3</b>	<b>1.9</b>	
<b>OTHER</b>				
Open Space (OS)	375.94	17.4		
Public Facilities/Schools (PF)	84.30	3.9		
Right-of-Way (ROW)	175.5	8.1		
<b>Total Other</b>	<b>641.44</b>	<b>29.4</b>		
<b>TOTAL</b>	<b>2,162.65</b>	<b>100%</b>	<b>100%</b>	<b>7,162</b>

1. Assumes total of 1,526.91 acres of developable land. This does not include land uses in "other" category.
2. The ROW acreage includes local streets, which are not depicted on the Land Use Plan. See **Figure 3.1-2, Circulation Plan**.
3. This number represents a maximum, rather than a target.

Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
E-2 <sup>2</sup>	Mixed Use (MU)		148.74	
E-3	Open Space (OS)		69.33	
E-4	Mixed Use (MU)		12.25	
E-5	Mixed Use (MU)	19 <sup>1</sup>	5.31	
E-6	Mixed Use (MU)	120 <sup>1</sup>	33.99	
E-7	Mixed Use (MU)	5 <sup>1</sup>	1.32	
E-8	Mixed Use (MU)	15 <sup>1</sup>	4.94	
F-2	Medium Density Residential (MDR)	158	71.64	2.0-5.0
F-3	Medium High Density Residential (MHDR)	635	79.94	5.0-8.0
F-4	Commercial Retail (CR)		6.23	
G-1	Commercial Retail (CR)		4.43	
G-2	Open Space (OS)		1.44	
G-3	Open Space (OS)		1.19	
G-4	Open Space (OS)		5.6	
G-5	Medium Density Residential (MDR)	69	34.6	2.0-5.0
G-6	Open Space (OS)		13.65	
G-7	Medium High Density Residential (MHDR)	432	60.38	5.0-8.0
G-8	High Density Residential (HDR)	282	20.6	8.0-14.0
G-9	Open Space (OS)		6.17	
G-10	Medium Density Residential (MDR)	37	18.57	2.0-5.0
G-11	Medium Density Residential (MDR)	70	26.86	2.0-5.0





Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
M-4	Public Facility (PF)		84.3	
M-5A	Medium Density Residential (MDR)	14	4.1	2.0-5.0
M-5B	Medium Density Residential (MDR)	61	15.7	2.0-5.0
M-6A	Medium Density Residential (MDR)	14	3.8	2.0-5.0
M-6B	Medium Density Residential (MDR)	57	13.4	2.0-5.0
M-7A	Medium Density Residential (MDR)	20	4.1	2.0-5.0
M-7B	Medium High Density Residential (MHDR)	56	11	5.0-8.0
M-7C	Medium High Density Residential (MHDR)	32	5.9	5.0-8.0
M-7D	Medium High Density Residential (MHDR)	82	15.1	5.0-8.0
M-7E	Medium High Density Residential (MHDR)	82	15.5	5.0-8.0
M-8	Open Space (OS)		2.9	
M-9	Open Space (OS)		2.7	
Right-of-Way			175.5	
<b>TOTALS</b>		<b><u>7.162</u></b>	<b><u>2.162.65</u></b>	

Note: Planning Area's B-4, B-7, C-3 and C-7 have been intentionally left out.

1. This number represents a maximum rather than a target.

2. This planning area allows for 110 racetrack recreational units

**Mixed Use**

The Mixed Use designation is located in the northernmost portion of the project site, just south of the Jacqueline Cochran Regional Airport. Approximately 252.73 acres are devoted to this land use. The purpose of this designation is to accommodate a wide range of land uses including office, service, commercial, airport-related, incubator business, mixed-use residential units, very light industrial, and large scale recreational development. The intent is to: 1) complement expansion plans for the Jacqueline Cochran Regional Airport by providing needed services and commercial uses located near the Tyler Street airport entrance; 2) devise standards that encourage combinations of uses such as office/sales/storage/assembly; 3) provide support services for business and industrial uses in the project area; and 4) provide standards unique to development of large scale recreational use.

In addition to General Commercial and Manufacturing-Heavy development, additional uses within the Mixed Use/Air Park land use designation may include such uses as above ground natural gas storage less than 6,000 gallons; aerial service businesses including advertising, photography and tours; aircraft equipment sales, service and repair; contractor storage yards; aircraft taxiways; aviation equipment assembly; breweries, distilleries and wineries; catering services/flight kitchens; community centers; computer and office equipment sales, service, repair and assembly; conference facilities; convenience stores; country clubs; dry cleaners; emergency and urgent care medical facilities; facilities for research and development of precision components and products; flight schools; hospitals; hotels and motels; ice houses; intermodal cargo transfer facilities; jewelry manufacture and repair; facilities related to large scale recreational uses such as a motor sports race track and facilities related thereto, including but not limited to race track, private garages, single-family residential including duplex units defined as a structure with two dwelling units placed beside one another sharing a common wall, racetrack recreational units, clubhouse, tuning shop, observation tower, museum, vehicle display areas, underground fuel storage and ancillary uses in support thereof; libraries; manufacture and repair of measuring devices, watches, clocks and related items; manufacture and repair of optical goods, medical instruments, supplies and equipment, engineering, survey and drafting instruments and photography equipment; manufacture and repair of refrigeration and heating equipment; manufacture of bicycles; manufacture of confectionery products; manufacture of dairy products, not including dairies; manufacture of furniture and fixtures, including cabinets, partitions, and similar small items; manufacture of grain and bakery products; manufacture of cutlery, tableware, hand tools and hardware; manufacture of non-alcoholic beverages; manufacture of office and computing machines; manufacture of plumbing and heating items; manufacture of wearing apparel and accessories; manufacture, assembly, testing and repair of components, devices, equipment and systems of an electrical, electronic, or electro-mechanical nature; outdoor film studio; paper storage and recycling within a building; parcel delivery services; public parks and public playgrounds; religious institutions; rental car agencies including the storage of rental cars; research and development facilities for biomedical, chemical, electronic, mechanical and other scientific purposes; research and manufacture of drugs and pharmaceuticals; schools; warehousing and distribution; water wells and appurtenant facilities. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

Due to the unique nature of large scale recreational development north of Avenue 62, the Mixed Use designation allows for two or more different types of uses contiguous to one another, planned as a unit. For instance, with racetrack development north of Avenue 62, certain

### 3.2.5 Neighborhood E

Neighborhood E is situated in the northeastern corner of the project site and is bounded by Jacqueline Cochran Regional Airport and Avenue 60 on the north, Polk Street on the east, and Avenue 62 on the south as depicted in **Figure 3.2-6, Neighborhood E**. This neighborhood has a strong regional orientation as a result of its excellent access to the regional circulation system and to Jacqueline Cochran Regional Airport. The neighborhood also looks eastward by virtue of its potential connection to the proposed interchange at Avenue 62 and the new Highway 86, just a few miles to the east. Primary access is provided by Polk Street, Avenue 60 and Avenue 62. The land uses proposed in this 281.49-acre neighborhood include mixed-use (206.55 acres), and open space (74.94 acres).

A racetrack has been approved for this neighborhood with its own separate set of conditions of approval and design manual for features specific to its development. The open space in the northeastern portion of the neighborhood responds to airport-related constraints, and will become part of the larger recreational land use.

Land designated for mixed use development abutting the airport property offers the potential for airport uses to extend southward into the site. This area also has potential for rail service to be provided from the north via a rail spur from the Southern Pacific main line. In addition to the jobs that could be created by heavy industry, Neighborhood E allows for a large-scale recreational use such as a motor sports race track which will also create jobs. Agricultural operations to the east of Polk Street offer the potential for development of compatible agriculture-related uses on the property. A Major Entry is located at the intersection of Polk Street and Avenue 62.

#### NEIGHBORHOOD E PLANNING STANDARDS

- (1) Portions of Neighborhood E are constrained by the OSZ and ETZ airport safety zones as reflected in **Figure 3.1-13, Airport Zones**. Additionally, the neighborhood is located within Zones B1, C, and D of the CLUP as reflected in in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to these zones are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan. As Neighborhood E allows for mixed-use residential uses in designated planning areas, **Figure 3.2-6A, Neighborhood E Mixed Use Residential Lots**, identifies which lots within those planning areas allow for such use. Where a residential lot overlaps two airport land use compatibility zones, the least restrictive zone policy applies. For example, Lot 55 on Figure 3.2-6A lies within both Zone C and D. Because Zone D is the less restrictive zone, Zone D ALUC policies apply.
- (2) ~~Avenue 62-Tyler Street~~ provides access to Planning Areas E-2, E-3, E-4, E-5, E-6, E-7, and E-8 ~~until such time as access is developed as part of through~~ Planning Area A-6.
- (3) Avenue 60 provides access to Planning Area E-1 and E-2.
- (4) Polk Street provides access to Planning Area E-2, E-5, E-6, E-7, and E-8.
- (5) A Major Project Entry is located at ~~Avenue 62-Tyler Street~~ and Polk Street to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- ~~(6) A Special Intersection Detail is located at "C" Street and Avenue 62 to be developed in accordance with Figures 3.4-5, Intersection Detail.~~
- ~~(7)~~(6) Polk Street will be developed in accordance with **Figure 3.4-9, Arterial Streetscape 2 – Polk Street Streetscape 2A**.
- ~~(8)~~(7) Avenue 62 will be developed in accordance with **Figure 3.4-6, Expressway Streetscape**, with the exception of the ETZ in which no trees are to be planted.
- ~~(9)~~(8) A large-scale recreational use such as a motor sports race track is a permitted land use in seven of the eight planning areas as defined in Section 2 – Specific Plan Zoning Ordinance.

~~(10)(9)~~ The north side of Avenue 62 will be developed as depicted in **Figure 3.1-10, Public Facilities** in accordance with **Figure 3.4-20, Avenue 62 Trail**.

~~(11)(10)~~ The west side of Polk Street will be developed with a Class II Bike Path as described **Figure 3.1-10, Public Facilities**.

**NEIGHBORHOOD E PLANNING AREAS****E-1: OPEN SPACE**

Planning Area E-1 provides for the development of 5.61 acres of open.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area E-1 is located within Zone B1 of the Jacqueline Cochran Regional Airport CLUP and the ETZ and OSZ airport safety zones. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Large-scale recreational uses such as a motor sports race track are permitted uses in this planning area.
- (3) Planning Area E-1 is accessed from Avenue 60.
- (4) Large-scale recreational uses such as a golf course and a motor sports race track are permitted uses in this planning area.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

**E-2: MIXED USE**

Planning Area E-2 provides for the development of 148.74 acres of mixed-use ~~nonresidential~~ development.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area E-2 is located within Zone C of the Jacqueline Cochran Regional Airport CLUP. This planning area will comply with all applicable restrictions and requirements of that zone.
- (2) Access to Planning Area E-2 is from Tyler and Polk Streets.
- (3) The edge treatment for the interface between the planned mixed use and adjacent open space is illustrated in **Figure 3.4-28, Airpark Drainage Buffer**. If the race track is developed, the edge treatment will be as described in **Figure 3.4-24, Golf Course/Racetrack Edge Condition**.
- (4) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) Large-scale recreational uses such as a motor sports race track are permitted uses in this planning area ~~including racetrack recreational units as defined below. Signage shall be posted that overnight occupancy is prohibited in non-residential structures.~~
- (6) ~~Standards for Racetrack Recreational Unit development:~~
  - a. ~~A maximum of 110 racetrack recreational units with minimum lot sizes of 7,000 square feet, as defined in Section 2-Specific Plan Zoning, may be provided in Planning Area E-2.~~
  - b. ~~No buffer is required for uses contiguous to this Planning Area.~~
  - c. ~~Standard Setbacks~~
    - i. ~~No minimum setback is required from any private street.~~
    - ii. ~~Front Yard: 30-foot minimum~~
    - iii. ~~Rear Yard: No minimum~~

- iv. Side Yard: 5-feet.  
 d. Structures must provide vehicle storage area on the ground floor of structure and may include the following:

Garage Level (1<sup>st</sup> Floor):

- Cars
- Lifts
- Work areas
- Storage
- Mechanical
- Main Entry
- Half baths
- Bath with locker rooms
- Washer/dryer area
- Elevator
- Stairs
- Media Room
- Kitchenette area

Second Level (and/or mezzanine):

- Elevator
- Stairs
- Storage
- Mechanical
- Bedrooms and closets
- Bath area
- Laundry
- Family Room/Game Room
- Kitchen
- Outside Patios
- Outside Planters

Third Level (Main Living Level):

- Bedrooms and Closets
- Bath
- Elevator
- Stairs
- Laundry
- Kitchen
- Dining
- Great Room
- Outside Patios
- Mechanical
- Storage
- Game Room
- BBQ area
- Outside pools and hot tub
- Planters

- e. Access to structures shall be from common access areas or corridors

~~(5)~~ \_\_\_\_\_

~~(6)~~(7) No minimum setback or buffer is required for Planning Area E-2 from adjacent residential uses.

~~(7)~~(8) An observation tower built within Planning Area E-2 and built as part of a large scale recreational use shall not exceed 70 feet.

- ~~{8}~~{9} All lighting fixtures, including spot lights, electrical reflectors and other means of illumination for signs, structures, landscaping, parking, loading, unloading and similar areas, shall be focused, directed, and arranged to prevent glare or direct illumination on streets or adjoining property.
- ~~{9}~~{10} Sports lighting, consisting of exterior nighttime lighting for ballfields, racetracks, and other sporting activities, shall not be permitted.
- ~~{10}~~{11} Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- ~~{11}~~{12} Please refer to Section 3.1 – Project Wide Development Standards.

|

EXCERPT: AMENDED ZONING ORDINANCE PROVISIONS FOR PLANNING AREA E-2  
(ADDED TEXT UNDERLINED)



1 minimum setback from any private street. Article XII, Section 12.4c.(2) is modified to provide that  
2 an observation tower built within Planning Areas A-4 and built as part of a large scale recreational  
3 use shall not exceed 70 feet in height and sports lighting, consisting of exterior nighttime lighting  
4 for ballfields, racetracks and other sporting activities, shall not be permitted.

5 (3) Except as provided above, all other zoning requirements shall be the same as  
6 those requirements identified in Article XII of Ordinance No.348.

7 i. Planning Area A-6, E-2, and E-4

8 (1) The uses permitted in Planning Areas A-6 and E-2 of Specific Plan 303 shall be  
9 the same as those uses permitted in Article IX, Section 9.1 and Article XII, Section 12.2 of  
10 Ordinance No. 348, except that the uses permitted pursuant to Article IX, Sections 9.1.a. (17), (23),  
11 (25), (27), (29), (32), (42), (51), (52), (61), (65), (67), (73), (83), (93) and (96-within Airport Land  
12 Use Compatibility Zone C); Sections 9.1.b. (7), (9), (10), (11.a.) (11.b), (11.c), (13), (14), (15),  
13 (16), (18), (19), and (20); and Sections 9.1.d. (2), (4), (5), (6), (9), (10), (11) (12), (13), and (16)  
14 and to Article XII, Section 12.2.c. (3), (7), (11) and (12); Section 12.2.d.; Section 12.2.e., 12.2.f.  
15 and 12.2.g. shall not be permitted; and uses permitted pursuant to Article IX, Section 9.1.a (35)  
16 shall not be permitted in Planning Areas E-2 and E-4.

17 In addition, the permitted uses identified under Section 9.1.a and 12.2b shall include aerial  
18 services including advertising, photography and tours; aerospace/aeronautical museums; aircraft  
19 equipment sales, service, and repair; aircraft taxiways; aviation equipment assembly; catering  
20 services/flight kitchens; conference facilities; computer and office equipment sales, service, repair  
21 and assembly; conference facilities; contractor storage yards; convenience stores; country clubs;  
22 dry cleaners; emergency and urgent care medical facilities; facilities related to large scale  
23 recreational uses such as a motor sports race track and facilities related thereto, including but not  
24 limited to race track, private garages, clubhouse, tuning shop, observation tower, museum, vehicle  
25 display areas, underground fuel storage and ancillary uses in support thereof; flight schools;  
26 ~~hospitals~~; ice houses; intermodal cargo transfer terminals; ~~libraries~~; manufacture of dairy products,  
27 not including dairies; manufacture of grain and bakery products; manufacture and repair of jewelry;  
28 manufacture of wearing apparel and accessories; manufacture and repair of measuring devices,

1 watches, clocks and related items; manufacture and repair of optical goods, medical instruments,  
2 supplies and equipment, engineering, survey and drafting instruments and photography equipment;  
3 manufacture of furniture and fixtures, including cabinets, partitions and similar small items;  
4 manufacture of cutlery, tableware, hand tools and hardware; manufacture of plumbing and heating  
5 items; manufacture of office and computing machines; manufacture, assembly, testing and repair of  
6 components, devices, equipment and systems of an electrical, electronic, or electro-mechanical  
7 nature; manufacture of non-alcoholic beverages; manufacture of confectionery products;  
8 manufacture and repair of refrigeration and heating equipment; outdoor film studio; paper  
9 recycling facilities; parcel delivery services; public parks and public playgrounds; ~~religious~~  
10 ~~institutions;~~ rental car agencies including the storage of rental cars; research and development  
11 facilities for biomedical, chemical, electronic, mechanical and other scientific purposes; research  
12 and development facilities for precision components and products; and water wells and appurtenant  
13 facilities

14 In addition, the permitted uses identified under Section 9.1.a and 12.2b for Planning Area  
15 E-2 shall include racetrack recreational units.

16 In addition, the permitted uses identified under Section 9.1.d. and Section 12.2.c shall  
17 include above ground natural gas storage less than 6,000 gallons; breweries, distilleries and  
18 wineries; community centers, research and manufacture of drugs and pharmaceuticals; and paper  
19 storage and recycling within a building.

20 (2) The development standards for Planning Areas A-6 and E-2 of Specific  
21 Plan No. 303 shall be the same as those standards identified in Article XII, Section 12.4 of  
22 Ordinance No. 348. Provided however that Article XII, Section 12.4(A) is modified to provide that  
23 the minimum lot area shall be seven thousand (7,000) square feet with no minimum average width.  
24 There shall be no setback from any private street. Article XII, Section 12.4(B)(3) shall apply only  
25 to setbacks from calculated public streets. Article XII, Section 12.4(C)(2) is modified to provide  
26 that an observation tower built within Planning Areas A-6 and E-2 and built as part of a large scale  
27 recreational use shall not exceed 70 feet in height. Article XII, Section 12.4(K) is modified to  
28

1 provide that sports lighting, consisting of exterior nighttime lighting for ballfields, racetracks and  
2 other sporting activities, shall not be permitted.

3 (3) A maximum of 110 racetrack recreational units in Planning Area E-2 shall be permitted.

4 The following development standards shall apply to the recreational racetrack units:

- 5 a) Minimum lot sizes of 7,000 square feet.
- 6 b) No minimum setback is required from any private street.
- 7 c) Front yard setback shall be a minimum of thirty feet (30').
- 8 d) There shall be no rear yard setback.
- 9 e) Side yard setback shall be a minimum of five feet (5').
- 10 f) No buffer is required for uses contiguous to this Planning Area.
- 11 g) Structures shall provide vehicle storage area on the ground floor of structure.
- 12 h) Access shall be from common access areas or corridors
- 13 i) First floor of structure may include any of the following: cars, lifts, work areas,  
14 storage, mechanical, main entry, half baths, bath with locker rooms, washer/dryer  
15 area, elevator, stairs, media room, kitchenette area.
- 16 j) Second floor of structure may include any of the following: elevator, stairs,  
17 storage, mechanical, bedrooms and closets, bath area, laundry area, family  
18 room/game room, kitchen, outside patios, outside planters.
- 19 k) Third floor of structure may include any of the following: bedrooms and closets,  
20 bath, elevator, stairs, laundry area, kitchen, dining area, great room, outside patios,  
21 mechanical, storage, game room, barbeque area, outside pools and hot tubs, outside  
22 planters.
- 23 l) Overnight accommodations shall be limited to forty-five (45) consecutive nights  
24 my any owner, visitor, or any occupant including but not limited to any vehicle  
25 maintenance staff, housekeeping staff, or any form of groundskeeper. The Home  
26 Owner Association shall be responsible for the enforcement of this provision.

27 (34) Except as provided above, all other zoning requirements shall be the same as those  
28 requirements identified in Article IX and Article XII of Ordinance No. 348.

## Proposed Addendums to CC&Rs and Club Rules & Regulations

### CC&R's

#### ARTICLE 6 – RESTRICTIONS

6.16 “Residential Uses and Overnight Occupancy. Without limiting Section 4.1 above, (a) no portion of the Property shall be used as a primary residence and (b) ~~residential uses and~~ overnight occupancy shall be permitted only on those portions of the Property identified on Exhibit C attached hereto, but such ~~residential use and~~ overnight occupancy shall be limited to ~~ninety (90)~~ forty-five (45) consecutive nights by any owner, ~~renter, visitor or~~ any occupant, including but not limited to any vehicle maintenance staff, housekeeping staff, or any form of grounds keeper. Should any occupant stay longer than forty-five (45) consecutive nights, the owner of the property shall, at 12:01 am the forty-sixth (46th) day immediately lose track rights and club privileges. Upon the 50th (fiftieth) day, the property owner will be subject to fines, membership revocation, and/or additional sanctions imposed by the Declarant or OpCo until the violation is remedied. Track rights and club privileges shall be restored upon vacating the property and/or addressing the incurred penalty (penalties).”

#### ARTICLE 8 – DURATION; MODIFICATION

8.2 Amendment. This Declaration may be amended from time to time, by an instrument in writing, properly executed and acknowledged by the Declarant; except that Section 6.16 shall not be revised without the approval of the County of Riverside Planning Director.

#### ARTICLE 13 – GENERAL COVENANTS AND RESTRICTIONS

13.10 Accountability Report. OpCo shall prepare and submit to the Riverside County Airport Land Use Commission (ALUC) a quarterly report which shall provide a summary of maximum consecutive night stays, minimum consecutive night stays, average overnight visits, violations incurred and warnings issued for Founder Lots 91 to 201 of Parcel Map No. 36293-1 during the reporting period. In the event an Accountability Report is not provided to ALUC, the Riverside County Code Enforcement Department may take appropriate action for violation.

#### The Thermal Club Rules and Regulations

Addition of #21 to General Club Rules:

21. Overnight Stays. No portion of the Property shall be used as a primary residence; overnight Overnight occupancy shall be permitted only on those lots approved for overnight stays. Such residential use and overnight occupancy and shall be limited to forty-five (45) consecutive nights by any owner, ~~renter, visitor, or~~ any occupant, including but not limited to any vehicle maintenance staff, housekeeping staff, or any form of grounds keeper. Should any occupant stay longer than forty-five (45) consecutive nights, the owner of the property shall, at 12:01 am the forty-sixth (46th) day immediately lose track rights and club privileges. Upon the 50th (fiftieth) day, the property owner will be subject to fines, membership revocation, and/or additional sanctions imposed by the Declarant or OpCo until the

violation is remedied. Track rights and club privileges shall be restored upon vacating the property and/or addressing the incurred penalty (penalties).



# RIVERSIDE COUNTY

## AIRPORT LAND USE COMMISSION

### APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP 1039 TH17      DATE SUBMITTED: 10-5-17

#### APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

<b>Applicant</b>	Thermal Operating Company, LLC c/o Tim Rogers	Phone Number 310-486-4774
<b>Mailing Address</b>	1983 W. 190th Street, Suite 100 Torrance, CA 90504	Email timr@towerenergy.com
<b>Representative</b>	Albert A. Webb Associates c/o Fayres Hall	Phone Number 951-686-1070
<b>Mailing Address</b>	3788 McCray Street Riverside, CA 92506	Email fayres.hall@webbassociates.com
<b>Property Owner</b>	JTM Land Company, LLC	Phone Number 310-486-4774
<b>Mailing Address</b>	1983 W. 190th Street, Suite 100 Torrance, CA 90504	Email timr@towerenergy.com

U. C. C. D.

#### LOCAL JURISDICTION AGENCY

<b>Local Agency Name</b>	Riverside County	Phone Number 955-2009
<b>Staff Contact</b>	Ken Beaz	Email kbaez@rivco.org
<b>Mailing Address</b>	4080 Lemon Street 12th Floor Riverside CA 92056	Case Type
<b>Local Agency Project No</b>	SPA00303A4 CZ07952	<input checked="" type="checkbox"/> General Plan / Specific Plan Amendment <input checked="" type="checkbox"/> Zoning Ordinance Amendment <input type="checkbox"/> Subdivision Parcel Map / Tentative Tract <input type="checkbox"/> Use Permit <input type="checkbox"/> Site Plan Review/Plot Plan <input type="checkbox"/> Other

#### PROJECT LOCATION

*Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways*

<b>Street Address</b>	North of 62nd Avenue, south of Avenue 60, east of Tyler Street and west of Polk Street.		
<b>Assessor's Parcel No.</b>	See attached.	<b>Gross Parcel Size</b>	148.74
<b>Subdivision Name</b>	PM36293-1	<b>Nearest Airport and distance from Airport</b>	
<b>Lot Number</b>			

#### PROJECT DESCRIPTION

*If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed*

<b>Existing Land Use (describe)</b>	Mixed use

Proposed Land Use (describe)	Specific Plan Amendment for land use change for planning area E-2 of the Khol Ranch Specific Plan No. 303 to allow for racetrack recreational units.		
	Change of Zone (Zoning text is changing) to allow for racetrack recreational units in planning area E2.		
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units) _____		
For Other Land Uses	Hours of Operation	TBD	
(See Appendix C)	Number of People on Site	TBD	Maximum Number TBD
	Method of Calculation	TBD	
Height Data	Site Elevation (above mean sea level)	TBD	ft.
	Height of buildings or structures (from the ground)	TBD	ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?		<input type="checkbox"/> Yes
			<input checked="" type="checkbox"/> No
	If yes, describe _____		
	_____		
	_____		

- A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:**
1. . . . . Completed ALUC Application Form
  1. . . . . ALUC fee payment
  1. . . . . Plans Package (24x36 folded) (site plans, floor plans, building elevations, landscaping plans, grading plans, subdivision maps)
  1. . . . . Plans Package (8.5x11) (site plans, floor plans, building elevations, landscaping plans, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
  1. . . . . CD with digital files of the plans (pdf)
  1. . . . . Vicinity Map (8.5x11)
  1. . . . . Detailed project description
  1. . . . . Local jurisdiction project transmittal
  3. . . . . Gummed address labels for applicant/representative/property owner/local jurisdiction planner
  3. . . . . Gummed address labels of all surrounding property owners within a 300 foot radius of the project site (**only required if the project is scheduled for a public hearing Commission meeting**). If more than 100 property owners are involved, please provide pre-stamped envelopes (size #10) with ALUC return address. \*

\* Projects involving heliports/helicopter landing sites will require additional noticing procedures.

## **Planning Area E-2 APN List**

Portion of 759-180-010

759-180-013

Portion of 759-180-014

759-190-011

759-190-013

Portion of 759-190-014

759-240-009 THRU -018

759-250-001 THRU -029, -032 THRU -040

759-260-001 THRU -036

759-260-037

759-260-038

759-270—001 THRU -027



# THE KOHL RANCH

COACHELLA VALLEY, CALIFORNIA

Specific Plan No. 303, Amendment No. **34**

PROJECT SPONSOR:

**THERMAL OPERATING COMPANY, LLC**

1983 W. 190<sup>TH</sup> STREET, SUITE 100

TORRANCE CA 90504

CONTACT PERSON: TIM ROGERS



LEAD AGENCY:

**COUNTY OF RIVERSIDE PLANNING DEPARTMENT**

82-678 HWY 111, 2<sup>ND</sup> FLOOR, ROOM 209

INDIO, CALIFORNIA 92201

CONTACT PERSON: **MATT STRAITER** **RUSSELL BRADY**



PREPARED BY:

**ALBERT A. WEBB ASSOCIATES**

3788 MCCRAY STREET

RIVERSIDE, CALIFORNIA 92506

CONTACT PERSON: MELISSA PEREZ

ORIGINAL SPECIFIC PLAN ADOPTED: NOVEMBER 16, 1999

AMENDMENT NO. 1 ADOPTED: JANUARY 28, 2003

AMENDMENT NO. 2 ADOPTED: JUNE 7, 2011

AMENDMENT NO. 3 ADOPTED: MARCH 24, 2015

**AMENDMENT NO. 4 DRAFT 1: SEPTEMBER 2017**



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## 1 EXECUTIVE SUMMARY

### 1.1 Introduction

#### 1.1.1 Purpose and Authority of the Specific Plan

The purpose of the Kohl Ranch Specific Plan is to guide development and to stimulate responsible design through customized regulations and guidelines. The original Kohl Ranch Specific Plan was adopted November 16, 1999, Amendment No. 1 was adopted January 28, 2003, and Amendment No. 2 was adopted June 7, 2011. The third amendment to the Specific Plan (SPA3) was prepared pursuant to the authority granted to the County of Riverside by the California Government Code, Title 7, Division 1, Chapter 3, Article 8, Sections 65450 to 65457.

#### 1.1.2 Purpose and Authority of the EIR

The Environmental Impact Report (EIR) addresses potential environmental impacts of the Specific Plan for the Kohl Ranch project in the County of Riverside. The California Environmental Quality Act (CEQA) requires that all state and local governmental agencies consider the environmental consequences of projects over which they have discretionary authority. When the original Kohl Ranch Specific Plan was adopted, an EIR was prepared and certified to satisfy CEQA, as set forth in Public Resources Code Section 21000, et seq., the State *CEQA Guidelines*, 14 California Administrative Code Section 15000, et seq., and the County of Riverside's *CEQA Guidelines*. The original EIR was certified on November 16, 1999. A draft Addendum is being prepared to review the impacts of changes being made to the original EIR as the result of proposed SPA3SPA4. The EIR is the public document designed to provide local and state governmental agency decision makers with an analysis of environmental effects of the proposed project, to indicate possible ways to reduce or avoid environmental damage through mitigation measures and alternatives. The EIR also must disclose significant environmental impacts that cannot be avoided; growth-inducing impacts; effects not found to be significant; and significant cumulative impacts of all past, present, and reasonably foreseeable future projects.

#### 1.1.3 Scope of the EIR

As noted above, the EIR is an informational document used in local and state agency decision-making processes. It is not the purpose of the EIR to either recommend approval or denial of a project or to present political, social, or economic reasons to project approval or denial. Pursuant to CEQA, the County of Riverside served as the Lead Agency for the original EIR and prepared an Initial Study (see Technical Appendix A). For SPA3SPA4, an ~~third~~ Initial Study was conducted to analyze the potential environmental impacts of SPA3SPA4 as compared to the impacts studied in the EIR for the original Specific Plan (see Technical Appendix A). The County determined through the Initial Study for the original project that the adoption of a Specific Plan for the Kohl Ranch project may have significant adverse environmental impacts and that an EIR is required. The Initial Study for the original Specific Plan identified those environmental issues that may be significantly impacted by this project and are addressed in this EIR. The Initial Study for

SPA3SPA4 did not identify any environmental issues associated with SPA3SPA4 that were not previously identified in the original EIR. These issues include:

- General Plan Land Use Determination
- Land Use Element Consistency
- Existing Land Use and Zoning
- Landform and Topography/Slopes and Erosion
- Soils and Agriculture
- Biology
- Geology and Seismicity
- Hydrology, Flooding, and Drainage
- Air Quality
- Water Quality
- Noise
- Energy Resources
- Open Space and Conservation
- Toxic Substances
- Cultural Resources
- Aesthetics, Visual Analysis, Light and Glare
- Circulation and Traffic
- Water and Sewer
- Fire Services
- Sheriff Services
- Schools
- Parks and Recreation
- Utilities
- Solid Waste
- Health Services
- Disaster Preparedness
- Libraries
- Airports
- Housing Element
- Regional Element
- Administrative Element

**Intended Uses of the EIR**

On October 14, 1994, the County of Riverside, in its role as Lead Agency for this project, issued a Notice of Preparation (NOP) to the State Clearinghouse, responsible agencies, and other interested parties. The NOP and comments resulting from the distribution of the NOP are contained in Technical Appendix A.

Applications covered by the EIR Addendum, are as follows:

- Specific Plan 303 Amendment No. 43 (SP00303A3SP00303A4)
- Change of Zone No. 7852-XXXX (CZ007852CZ): modifies the Specific Plan Zoning Ordinance to accommodate changes to planning areas and designations, and changes to zoning and design standards in the existing planning areas.

Commented [m1]: To be updated upon formal change of zone submittal

A lead agency is the agency with primary responsibility for approval of the project. Other agencies having discretionary approval over a project are "Responsible Agencies" under CEQA. This document will provide environmental information for several other agencies affected by the



project, or which are likely to have an interest in the project. Various state and federal agencies exercise control over certain aspects of the project area. The various public, private, and political agencies and jurisdictions with a particular interest in the proposed project include the following:

#### Federal Agencies

- *Fish and Wildlife Service (FWS)* – Responsible for conserving and protecting wild birds, endangered species, and their habitat.
- *Federal Highway Administration (FHWA)* – Responsible for approving changes to the interstate freeway system.
- *Environmental Protection Agency (EPA)* – Responsible for administration of the Superfund program.

#### State Agencies

- *California Department of Fish and Wildlife (CDFW)* – Responsible for the protection, conservation, propagation, and enhancement of California's wildlife resources. This department enforces laws and regulations, and issues licenses relative to and cooperates with local agencies in developing projects. This agency will act as a Trustee.
- *California Regional Water Quality Control Board (CRWQCB)* – Responsible for evaluating appropriate uses of water and for issuing National Pollution Discharge Elimination System (NPDES) permits and waste discharge requirements.
- *California Reclamation Board (CRB)* – Responsible for delineation of flooding and regulation of encroachments into designated floodways.
- *California Department of Transportation (Caltrans)* – Responsible for approval of roadway improvements along state highways, including State Routes 86 and 195.
- *California Environmental Protection Agency (CALEPA)* – This agency is the primary state agency concerned with degradation of the environment and how it affects human health. It is responsible for the examination and prevention of pollution of sources of public water supplies; establishment of ambient standards of air quality; monitoring of environmental pollution, regulation of the quality of water supplies and sewage disposal systems; regulation of hazardous waste; regulation of pesticides; regulation and control of radioactive materials; and providing certain laboratory support to other state agencies.
- *California Department of Toxic Substances Control (DTSC)* – This CALEPA agency is the primary state agency that regulates matters related to hazardous waste. It is responsible for the cleanup of hazardous waste sites and permitting, surveillance and enforcement of hazardous waste facilities.
- *State Air Resources Board (CARB)* – This CALEPA agency is responsible for ensuring implementation of the California Clean Air Act, responding to the Federal Clean Air Act and for regulating emissions from consumer products and motor vehicles.
- *California Department of Conservation* – This agency reviews projects for their impacts on agricultural resources.

#### Local Agencies

- *County of Riverside* – Responsible for land use control, and the provision of urban services on and to the project site. The County will act as the Lead Agency for the proposed project.
- *South Coast Air Quality Management District (SCAQMD)* – Has responsibility for the implementation of the California Clean Air Act. This agency's authority includes Los Angeles and Orange Counties and the western portion of Riverside County.

- *Southern California Association of Governments (SCAG)* – Stimulates intergovernmental cooperation in planning and development activities, and assures better coordination of federally assisted projects. Reviews applications of local and regional agencies for federal grants related to more than 100 programs. Responsible for preparing components of the California Regional Transportation Plan. Programs range from open space planning, waste control and water basin studies to aviation, housing and research in economics and demography.
- *Adjacent Cities* – The cities of Indio, Coachella, and La Quinta are located adjacent to the unincorporated portion of Riverside County where the project is located and will be affected by the proposed project.
- *Coachella Valley Water District (CVWD)* – Responsible for providing domestic water, sanitation, and regional stormwater protection for the site. This area is within District No. 1 of the Coachella Valley Water District for irrigation service. Water from the Coachella Canal is available and shall be used to irrigate golf courses and greenbelts. CVWD can also provide agricultural drainage to this area.
- *Coachella Valley Association of Governments (CVAG)* – Responsible for programming transportation improvements in the project area, preparing demographic forecasts, and solid waste and air quality planning.
- *Riverside County Airport Land Use Commission (ALUC)* – Responsible for reviewing land use proposals for consistency with the Comprehensive Land Use Plan for the Jacqueline Cochran Regional Airport.

Public agencies and interested parties, who did not respond to a request for comment during the preparation of the EIR, will have an opportunity to comment during the public review period for the Draft EIR.

## 1.2 PROJECT SUMMARY

### 1.2.1 Project Location

The 2,163.78-acre Kohl Ranch Specific Plan is located in the Coachella Valley portion of Riverside County as depicted by **Figure 1-1, Regional Location**. The site is just south of Jacqueline Cochran Regional Airport, and is roughly east of Harrison Street/Highway 86, west of Highway 111, and north of Highway 195<sup>1</sup>. The project is bounded by Avenue 60 on the north, Polk Street on the east, Avenue 66 on the South, and by a line approximately 3,900 feet west of Tyler Street on the west as depicted in **Figure 1-2, Project Vicinity**.

### 1.2.2 Site Description

The Kohl Ranch site is characterized by flat terrain, with a very gentle slope from northwest to southeast. Elevations range between approximately 125 and 164 feet below sea level. The majority of the site is currently in agricultural use, although a significant portion in the southern section is vacant, disturbed land with sparse, non-native vegetation as depicted in **Figure 1-3, Aerial Photograph**. Existing man-made features include the Avenue 64 Evacuation Channel which flows west to east through the project site, and structures associated with current and past farming activities, including the Kohl Ranch headquarters and an abandoned feed lot. Some limited residential uses occur along the project periphery and Avenue 61. Adjacent, off-site land uses include vacant land, farms and related uses, a former sludge processing operation<sup>2</sup>, residences, and the Jacqueline Cochran Regional Airport. The Torres Martinez Indian Reservation abuts Section 9 on the west, south, and east. These Native American lands are held in individual and tribal ownership.

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<sup>1</sup>Please note that for clarification purposes, this Specific Plan refers to the "old" Highway 86 as Harrison Street, its local street name. The designation SR-86S refers to the new freeway constructed east of the Whitewater River.

<sup>2</sup>On November 28, 1994, a U.S. District Court judge issued a preliminary injunction preventing more sludge from being brought to the site. A late March 2011 hearing has been scheduled to make the injunction permanent. Two companies composting sewage sludge announced in December 2010 that they are closing their operations at the site.

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1-6 Executive Summary

Section 1

Figure 1-1, Regional Location



Specific Plan Amendment No. 43



## 1-7 Executive Summary

Section 1

Figure 1-2, Project Vicinity

Specific Plan Amendment No. ~~43~~

1-8 Executive Summary

Section 1

Figure 1-3, Aerial Photo

### 1.2.3 Proposed Changes to Specific Plan No. 303

The Kohl Ranch Specific Plan No. 303, Amendment 3-4 (SPA3SPA4) is a result of a change in land use allowances located in the northeasterly area of the project site (specifically Planning Area E-2). SPA3-SPA4 includes the addition of outdoor film studios, and racing facility related residential racetrack recreational uses and live/work units as allowable land uses under the ~~new~~ Mixed Use land use designation for Planning Area E-2, ~~update of the zoning ordinance, adjustments to the design guidelines for the Mixed Use land use designation, and a more detailed specific plan implementation and administrative processing discussion.~~ Existing planning area E-2 has been divided into five new planning areas: E-2, E-5, E-6, E-7, and E-8. Existing planning areas A-8 and E-4 will be combined to create one planning area (E-4). Planning areas A-6, E-2, E-4, E-5, E-6, E-7, and E-8 will be designated Mixed Use to identify and describe land use restrictions and conditions relative to ALUC Safety zones, and identify the special development needs related to ~~a motorsports racing park.~~ An adjustment has been made to the zoning ordinance through a change of zone (CZ07852 CZXXXX), and is reflected in this Specific Plan document, ~~to define and include horizontal and vertical mixed use development and outdoor film studio uses to the Mixed Use designation.~~

~~Target densities have been adjusted for planning areas F-2, G-5, G-10, G-11, H-2, and H-4 to allow for mixed-use residential units in planning areas E-5, E-6, E-7, and E-8 so as not to exceed the maximum unit count of 7,171.~~

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### 1.2.4 Project Overview

The Kohl Ranch Specific Plan Amendment 3-4 consists of a balanced array of land uses including residential, business, commercial, industrial, open space/recreation, and public facilities as depicted in **Figure 1-4, Land Use Plan** and **1-5**). Both living and working opportunities will be available within the project. The residential portion includes target of 7,162 dwelling units but with a maximum not exceed 7,171 dwelling units distributed among four different density classifications on 1,140.29 acres, with a gross residential density of 4.03 dwelling units per acre<sup>3</sup>. Although the maximum density and the total number of dwelling units within the high density and very high density residential designations can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code. Approximately 375.94 acres of open space provide for passive and active recreation, including trails and parks. The plan also allows for the development of large-scale recreational uses such as a golf course and a motor sports race track, which are identified as allowable land uses.

<sup>3</sup> The overall gross density for residential areas was determined by dividing the total number of dwelling units by the acreage devoted to residential land uses, open space, public facilities, and local residential streets. The gross density—if determined by dividing the total number of dwelling units by the residential acreage—is 6.28. This density is reduced to 3.31, if the total site acreage is used.

**Table 1-A, Land Use Diagram Statistical Summary**

Land Use	Acreage	Percent of Total	Percent of Developable Area <sup>1</sup>	Target Dwelling Units
<b>RESIDENTIAL</b>				
Medium Density Residential (MDR)	501.33	23.2	32.8	1,652
Medium High Density Residential (MHDR)	467.76	21.6	30.6	3,245
High Density Residential (HDR)	140.35	6.5	9.2	1,629
Very High Density Residential (VHDR)	30.85	1.4	2.0	477
<b>Total Residential</b>	<b>1,140.29</b>	<b>52.7</b>	<b>74.7</b>	<b>7,003</b>
<b>INDUSTRIAL</b>				
Heavy Industrial (HI)	81.17	3.8	5.3	
<b>Total Industrial</b>	<b>81.17</b>	<b>3.8</b>	<b>5.3</b>	
<b>BUSINESS</b>				
Mixed Use/Air Park (MU/AP)	24.45	1.1	1.6	
Mixed Use	252.73	11.7	16.6	159 <sup>3</sup>
<b>Total Business</b>	<b>277.18</b>	<b>12.8</b>	<b>18.2</b>	<b>159<sup>3</sup></b>
<b>COMMERCIAL</b>				
Commercial-Retail (CR)	28.27	1.3	1.9	
<b>Total Commercial</b>	<b>28.27</b>	<b>1.3</b>	<b>1.9</b>	
<b>OTHER</b>				
Open Space (OS)	375.94	17.4		
Public Facilities/Schools (PF)	84.30	3.9		
Right-of-Way (ROW)	175.50	8.1		
<b>Total Other</b>	<b>641.44</b>	<b>29.6</b>		
<b>TOTAL</b>	<b>2,162.65</b>	<b>100%</b>	<b>100%</b>	<b>7,162</b>

1. Assumes total of 1,526.91 acres of developable land. This does not include land uses in "other" category.
2. The ROW acreage includes local streets, which are not depicted on the Land Use Plan.
3. This number represents a maximum rather than a target.

Specific Plan Amendment No. ~~43~~

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Section 1

Figure 1-4, Land Use Plan (Color)

Specific Plan Amendment No. 43

1-12 Executive Summary

Section 1

Figure 1-5, Land Use Plan (Black & White)

**Table 1-B, Land Use Summary by Planning Area**

Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
A-1	Open Space (OS)		0.63	
A-2	Mixed Use/Air Park (MU/AP)		24.45	
A-3	Open Space (OS)		10.77	
A-4	Heavy Industrial (HI)		81.17	
A-5	Commercial Retail (CR)		17.61	
A-6	Mixed Use (MU)		46.18	
A-7	Open Space (OS)		9.97	
A-8	Heavy Industrial (HI)		6.55	
B-1	Medium High Density Residential (MHDR)	210	30	5.0–8.0
B-2	Medium High Density Residential (MHDR)	177	25.24	5.0–8.0
B-3	Medium Density Residential (MDR)	117	39.01	2.0–5.0
B-5	Medium High Density Residential (MHDR)	229	32.70	5.0–8.0
B-6	Medium High Density Residential (MHDR)	183	26.18	5.0–8.0
C-1	Medium Density Residential (MDR)	122	40.77	5.0–8.0
C-2	Medium High Density Residential (MHDR)	218	31.16	5.0–8.0
C-4	Medium High Density Residential (MHDR)	349	49.83	5.0–8.0
C-5	Open Space (OS)		9.97	
C-6	Very High Density Residential (VHDR)	477	30.85	14.0–20.0
C-8	Medium High Density Residential (RM)	105	14.96	5.0–8.0
D-1	Open Space (OS)		110.66	
E-1	Open Space (OS)		5.61	
E-2 <sup>2</sup>	Mixed Use (MU)		148.74	
E-3	Open Space (OS)		69.33	
E-4	Mixed Use (MU)		12.25	
E-5	Mixed Use (MU)	19 <sup>1</sup>	5.31	
E-6	Mixed Use (MU)	120 <sup>1</sup>	33.99	
E-7	Mixed Use (MU)	5 <sup>1</sup>	1.32	
E-8	Mixed Use (MU)	15 <sup>1</sup>	4.94	
F-2	Medium Density Residential (MDR)	158	71.64	2.0–5.0

Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
F-3	Medium High Density Residential (MHDR)	635	79.94	5.0-8.0
F-4	Commercial Retail (CR)		6.23	
G-1	Commercial Retail (CR)		4.43	
G-2	Open Space (OS)		1.44	
G-3	Open Space (OS)		1.19	
G-4	Open Space (OS)		5.6	
G-5	Medium Density Residential (MDR)	69	34.6	2.0-5.0
G-6	Open Space (OS)		13.65	
G-7	Medium High Density Residential (MHDR)	432	60.38	5.0-8.0
G-8	High Density Residential (HDR)	282	20.6	8.0-14.0
G-9	Open Space (OS)		6.17	
G-10	Medium Density Residential (MDR)	37	18.57	2.0-5.0
G-11	Medium Density Residential (MDR)	70	26.86	2.0-5.0
G-12	Open Space (OS)		5.35	
G-13	Medium Density Residential (MDR)	53	14.17	2.0-5.0
H-1	Open Space (OS)		5.12	
H-2	Medium High Density Residential (MHDR)	118	18.65	5.0-8.0
H-3	Open Space (OS)		3.79	
H-4	Medium High Density Residential (MHDR)	141	22.4	5.0-8.0
H-5	Medium High Density Residential (MHDR)	45	7.83	5.0-8.0
H-6	Open Space (OS)		11.43	
H-7	Medium High Density Residential (MHDR)	151	20.99	5.0-8.0
H-8	High Density Residential (HDR)	189	13.55	8.0-14.0
H-9	Medium Density Residential (MDR)	66	20.03	2.0-5.0
H-10	Open Space (OS)		3.96	
I-1	Open Space (OS)		9.6	
I-2	Open Space (OS)		3.9	
I-3	Open Space (OS)		0.5	
I-4	Medium Density Residential (MDR)	35	7	2.0-5.0
I-5	Medium Density Residential (MDR)	51	10.6	2.0-5.0
I-6	Medium Density Residential (MDR)	42	9.6	2.0-5.0



Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
I-7	Medium Density Residential (MDR)	29	6.7	2.0-5.0
I-8	Medium Density Residential (MDR)	40	8	2.0-5.0
I-9	Medium Density Residential (MDR)	57	11.5	2.0-5.0
I-10	Medium Density Residential (MDR)	47	12.4	2.0-5.0
I-11	Medium Density Residential (MDR)	37	10.1	2.0-5.0
J-1	Medium Density Residential (MDR)	130	37.52	2.0-5.0
J-2	Open Space/ Lake (OS-LAKE)		22	
J-3	Open Space/Lake (OS-LAKE)		8.8	
J-4	Medium Density Residential (MDR)	189	43.36	2.0-5.0
J-5	Open Space (OS)		1	
J-6	Open Space (OS)		2.2	
J-7	Open Space (OS)		3.3	
J-8	Open Space (OS)		1.3	
K-1	Open Space/Lake (OS-LAKE)		22.4	
K-2	Open Space/Lake (OS-LAKE)		8.8	
K-3	Open Space (OS)		3.2	
L-1	High Density Residential (HDR)	1,158	106.2	8.0-14.0
L-2	Open Space (OS)		4.1	
L-3	Open Space (OS)		1.1	
M-1A	Medium Density Residential (MDR)	8	2.5	2.0-5.0
M-1B	Medium Density Residential (MDR)	68	18.9	2.0-5.0
M-1C	Medium Density Residential (MDR)	61	16.4	2.0-5.0
M-2	Open Space (OS)		1.9	
M-3	Open Space (OS)		1.6	
M-4	Public Facility (PF)		84.3	
M-5A	Medium Density Residential (MDR)	14	4.1	2.0-5.0
M-5B	Medium Density Residential (MDR)	61	15.7	2.0-5.0
M-6A	Medium Density Residential (MDR)	14	3.8	2.0-5.0
M-6B	Medium Density Residential (MDR)	57	13.4	2.0-5.0
M-7A	Medium Density Residential (MDR)	20	4.1	2.0-5.0
M-7B	Medium High Density Residential (MHDR)	56	11	5.0-8.0

Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
M-7C	Medium High Density Residential (MDR)	32	5.9	5.0-8.0
M-7D	Medium High Density Residential (MDR)	82	15.1	5.0-8.0
M-7E	Medium High Density Residential (MHDR)	82	15.5	5.0-8.0
M-8	Open Space (OS)		2.9	
M-9	Open Space (OS)		2.7	
Right-of-Way			175.50	
<b>TOTAL</b>		<b>7,161</b>	<b>2,162.65</b>	

Note: Planning Areas B-4, B-7, C-3, and C-7 have been intentionally left out.

- 1. This number represents a maximum, rather than a target.
- 2. This planning area allows for 110 racetrack recreational units

The business, commercial, and industrial land use categories will comprise 277, 28, and 81 acres, respectively. Commercial areas will serve the Kohl Ranch project as well as neighboring communities. Business and industrial uses will be oriented toward the Jacqueline Cochran Regional Airport as well as larger regional markets, and are intended to provide employment opportunities to project area residents. The land uses proposed for the Kohl Ranch Specific Plan are described in **Figure 1-5, Land Use Plan** and are briefly summarized in **Table 1-A**, above.

1  
2 ORDINANCE NO. 348.

3 AN ORDINANCE OF THE COUNTY OF RIVERSIDE

4 AMENDING ORDINANCE NO. 348 RELATING TO ZONING

5 The Board of Supervisors of the County of Riverside Ordains as follows:

6 Section 1. Section 4.2 of Ordinance No. 348, and Official Zoning Plan Map No. \_\_\_\_\_, as  
7 amended, are further amended by placing in effect in the Lower Coachella Valley District the zone or zones  
8 as shown on the map entitled “Change of Official Zoning Plan Amending Ordinance No. 348, Map No.  
9 \_\_\_\_\_, Change of Zone Case No. [78527952](#),” which map is made a part of this ordinance.

10 Section 2. Article XVIIa, of Section 17.87 of Ordinance No. 348 is amended to read as follows:

11 Section 17.87 SP ZONE REQUIREMENTS AND STANDARDS FOR SPECIFIC PLAN NO.

12 303.

13 a. Planning Areas C-5, D-1, G-2, G-3, G-4, G-6, G-9, G-12, H-1, H-3, H-6, H-10, I-1, I-2, I-  
14 3, J-2, J-3, J-5, J-6, J-7, J-8, K-1, K-3, L-2, L-3, M-2, M-3, M-8, and M-9.

15 (1) The uses permitted in Planning Areas C-5, D-1, G-2, G-3, G-4, G-6, G-9, G-12, H-  
16 1, H-3, H-6, H-10, I-1, I-2, I-3, J-2, J-3, J-5, J-6, J-7, J-8, K-1, K-3, L-2, L-3, M-2, M-3, M-8, and  
17 M-9 of Specific Plan No. 303 shall be the same as those uses permitted in Article VIIIe, Section  
18 8.100 of Ordinance No. 348 except that the uses permitted pursuant to Section 8.100.a(1) and (2)  
19 and Section b.(1) shall not be permitted. In addition, the permitted uses identified under Section  
20 8.100.a. shall include public parks; community centers; and when the gross acre of a lot is twenty  
21 (20) acres or greater, the uses identified under Article XIII, Section 13.1.b. of Ordinance No. 348  
22 shall also be included.

23 (2) The development standards for Planning Areas C-5, D-1, G-2, G-3, G-4, G-6, G-9,  
24 G-12, H-1, H-3, H-6, H-10, I-1, I-2, I-3, J-2, J-3, J-5, J-6, J-7, J-8, K-1, K-3, L-2, L-3, M-2, M-3,  
25 M-8, and M-9 of Specific Plan No. 303 shall be the same as those standards identified in Article  
26 VIIIe, Section 8.101 of Ordinance No. 348.

27 (3) Except as provided above, all other zoning requirements shall be the same as those  
28 requirements identified in Article VIIIe of Ordinance No. 348.

1           b.       Planning Areas A-1, A-3, A-7, E-1, and E-3.

2                   (1)       The uses permitted in Planning Areas A-1, A-3, A-7, E-1, and E-3 of Specific Plan  
3           No. 303 shall be the same as those uses permitted in Article VIIIe, Section 8.100 of Ordinance No.  
4           348 except that the uses permitted pursuant to Section 8.100.b.(1) shall not be permitted. In  
5           addition, the permitted uses identified under Section 8.100.a. shall include public parks;  
6           community centers; facilities related to large scale recreational uses such as a motor sports race  
7           track and facilities related thereto, including but not limited to race track, private garages,  
8           clubhouse, tuning shop, observation tower, museum, vehicle display areas and ancillary uses in  
9           support thereof; and when the gross area of a lot is twenty (20) acres or greater, the uses identified  
10          under Article XIII, Section 13.1.b. of Ordinance No. 348 shall also be included.

11                   (2)       The development standards for Planning Areas A-1, A-3, A-7, E-1, and E-3 of  
12          Specific Plan No. 303 shall be the same as those standards identified in Article VIIIe., Section  
13          8.101 of Ordinance No. 348.

14                   (3)       If Planning Areas A-1, A-3, A-7, E-1, and E-3 are developed with large scale  
15          recreational uses such as a motor sports race track and facilities related thereto, the development  
16          standards shall be the same as those identified in Article VIIIe., Section 8.101 of Ordinance No.  
17          348 except that the following development standards shall also apply:

18                           (A)       The minimum front yard setback for any building shall be 20 feet.

19                           (B)       The minimum side yard setback for any building shall be 5 feet.

20                   (4)       Except as provided above, all other zoning requirements shall be the same as those  
21          requirements identified in Article VIIIe of Ordinance No. 348.

22           c.       Planning Areas A-2.

23                   (1)       The permitted uses in Planning Areas A-2 of Specific Plan No. 303 shall be the  
24          same as those permitted in Article IX, Section 9.1 of Ordinance No. 348, except that the uses  
25          permitted pursuant to Sections 9.1.a. (17), (23), (25), (27), (29), (32), (42), (51), (52), (61), (65),  
26          (67), (73), and (93); Sections 9.1.b. (7), (9), (11)a., (18), (19), and (20); and Sections 9.1.d. (4), (5),  
27          (7), (10), (11), (12) and (13) shall not be permitted. In addition, the permitted uses identified under  
28          Section 9.1.a. shall include aviation equipment assembly; communication equipment and

1 microwave sales and installation; computer and office equipment sales, service, repair and  
2 assembly; conference facilities; country clubs, manufacture of dairy products, not including dairies;  
3 emergency and urgent care medical facilities; libraries; manufacture of grain and bakery products;  
4 health and exercise centers; hospitals; ice houses; jewelry manufacture and repair; manufacture of  
5 wearing apparel and accessories; manufacture and repair of measuring devices, watches, clocks and  
6 related items; manufacture and repair of optical goods, medical instruments, supplies and  
7 equipment, engineering, survey and drafting instruments and photography equipment; manufacture  
8 of handbags, luggage, footwear, and other personal leather goods; manufacture of cutlery,  
9 tableware, hand tools and hardware; manufacture of plumbing and heating items; vehicle storage  
10 and impoundment; manufacture of office and computing machines; manufacture, assembly, testing  
11 and repair of components, devices, equipment and systems of an electrical, electronic, or electro-  
12 mechanical nature; manufacture of non-alcoholic beverages; manufacture of confectionery  
13 products; manufacture and repair of refrigeration and heating equipment; printing of periodicals,  
14 books, forms, cards and similar items; public parks and public playgrounds; golf courses; religious  
15 institutions; facilities for research and development of precision components and products; and  
16 water wells and appurtenant facilities.

17 In addition, the permitted uses identified under Section 9.1.b. shall include aerial service  
18 businesses including advertising, photography and tours; aerospace/aeronautical museums; aircraft  
19 equipment sales, service and repair; contractor storage yards; flight schools; intermodal cargo  
20 transfer facilities; manufacture of furniture and fixtures, including cabinets, partitions and similar  
21 small items; manufacture of bicycles; parcel delivery services; warehousing and distribution;  
22 facilities related to large scale recreational uses such as golf courses and a motor sports race track  
23 and facilities related thereto, including but not limited to race track, private garages, clubhouse,  
24 tuning shop, observation tower, museum, vehicle display areas and ancillary uses in support  
25 thereof; and when the gross area of a lot is twenty (20) acres or greater, the uses identified under  
26 Article XIII, Section 13.1.b. of Ordinance No. 348 shall be included.

27 In addition, the permitted uses identified under Section 9.1.d. shall include community  
28 centers; schools; meat and poultry processing not including slaughtering or rendering of animals;

1 paper shredding facilities; research and manufacture of drugs and pharmaceuticals; manufacture of  
2 soaps, cleaners and toiletries; wrought iron fabrication; machine, welding and blacksmith shops;  
3 breweries, distilleries and wineries; paper storage and recycling within a building; recycling  
4 processing facilities; paper and paperboard mills; manufacture of containers and boxes; and above  
5 ground natural gas storage.

6 (2) The development standards for Planning Areas A-2 of Specific Plan No. 303 shall  
7 be the same as those standards identified in Article IX, Section 9.4 of Ordinance No. 348 except  
8 that sports lighting, consisting of exterior nighttime lighting for ballfields, racetracks and other  
9 sporting activities, shall not be permitted.

10 (3) Except as provided above, all other zoning requirements shall be the same as those  
11 requirements identified in Article IX of Ordinance No. 348.

12 d. Planning Areas C-6, G-8, H-8 and L-1.

13 (1) The uses permitted in Planning Areas C-6, G-8, H-8 and L-1 of Specific Plan No.  
14 303 shall be the same as those uses permitted in Article VII, Section 7.1 of Ordinance No. 348,  
15 except that the uses permitted pursuant to Section 7.1.a.(3), (4) and (10); Section 7.1.b(9); and  
16 Section 7.1.c(1) shall not be permitted.

17 In addition, the permitted uses identified under Section 7.1.b. shall include two family  
18 dwellings developed pursuant to Subsections AA. through DD. of this section; lakes, including  
19 those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water and  
20 noncommercial fishing; water wells and appurtenant facilities; and when the gross area of a lot is  
21 twenty (20) acres or greater, the uses identified under Article XIII, Section 13.1.a. and b. of  
22 Ordinance No. 348 shall also be included, except that the uses permitted pursuant to Section  
23 13.1.a.(15) shall not be permitted.

24 (2) The development standards for Planning Areas C-6, G-8, H-8 and L-1 of Specific  
25 Plan 303 shall be the same as those standards identified in Article VII, Sections 7.2 through 7.11  
26 except that the development standards set forth in Sections 7.3, 7.4, 7.5, 7.6, and 7.10 shall be  
27 deleted and replaced by the following:  
28

1                   A.       Lot area shall be not less than four thousand (4,000) square feet, unless  
2 cluster development subject to the development standards set forth in subsections AA.  
3 through DD. of this section is utilized. The minimum lot area shall be determined by  
4 excluding that portion of a lot that is used solely for access to the portion of a lot used as  
5 building site.

6                   B.       The front yard shall be not less than 16 feet, measured from the existing  
7 street line or from any future street line as shown on any Specific Plan of Highways,  
8 whichever is nearer the proposed structure.

9                   C.       The minimum average width of that portion of a lot to be used as a  
10 building site shall be forty feet (40'), with a minimum average depth of one hundred feet  
11 (100') unless cluster development subject to the development standards set forth in  
12 subsections AA. through DD. of this section is utilized. "Flag" lots shall not be permitted.

13                  D.       The minimum frontage of a lot shall be forty feet (40') except that lots  
14 fronting on knuckles or cul-de-sacs may have a minimum frontage of thirty feet (30')  
15 unless cluster development subject to the development standards set forth in subsections  
16 AA. through DD. of this section is utilized. Lot frontage along curvilinear streets may be  
17 measured at the building setback in accordance with zone development standards.

18                  E.       Side yards on interior and through lots shall be not less than ten percent  
19 (10%) of the width of the lot, but not less than three feet (3') in width in any event, and  
20 need not exceed a width of five feet (5') unless cluster development subject to the  
21 development standards set forth in subsections AA. through DD. of this section is utilized.  
22 Side yards on corner and reverse corner lots shall be not less than ten feet (10') from the  
23 existing street line or from any future street line as shown on any Specific Plan of  
24 Highways, whichever is nearer the proposed structure, upon which the main building sides  
25 unless cluster development subject to the development standards set forth in subsections  
26 AA. through DD. of this section is utilized. Where a zero lot line design is utilized, the  
27 alternate side yard shall be not less than ten feet (10') in width.  
28

1 F. The rear yard shall not be less than ten feet (10') unless cluster  
2 development subject to the development standards set forth in subsection AA. through DD.  
3 of this section is utilized.

4 G. Every main building erected or structurally altered shall have a lot or  
5 building site of not less than one thousand one hundred (1,100) square feet for each  
6 dwelling unit in such main building unless cluster development subject to the development  
7 standards set forth in subsections AA. through DD. of this section is utilized.

8 In addition, when a cluster development design is utilized, the following  
9 development standards shall be applicable:

10 AA. The minimum overall area for each individual unit within a two-  
11 family dwelling exclusive of the area set aside for street rights of way shall be two  
12 thousand (2,000) square feet.

13 BB. The minimum lot area for two-family lots used as a residential  
14 building site shall be two thousand (2,000) square feet. The minimum lot area  
15 shall be determined by excluding that portion of a lot that is used solely for access  
16 to the portion of a lot used as a building site. For each two family dwelling,  
17 common open space shall be provided equal to the difference between the lot area  
18 for such two family dwelling and eight thousand (8,000) square feet.

19 CC. Side yards on interior and through lots shall be not less than three  
20 feet (3') for one-story buildings; not less than ten feet (10') for two-story buildings;  
21 and not less than fifteen feet (15') for three-story buildings. Side yards on corner  
22 and reversed corner lots shall be not less than ten feet (10') from the existing street  
23 line as shown on any Specific Plan of Highways, whichever is nearer the proposed  
24 structure, upon which the main building sides, except that where the lot is less than  
25 fifty feet (50') wide the yard need not exceed twenty percent (20%) of the lot  
26 width.



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DD. The rear yard shall not be less than ten feet (10') for one-story buildings; not less than fifteen feet (15') for two-story buildings; and not less than twenty feet (20') for three-story buildings.

(3) Except as provided above, all other zoning requirements shall be the same as those requirements identified in Article VII of Ordinance No. 348.

e. Planning Areas B-1, B-2, B-5, B-6, C-2, C-4, C-8, F-3, G-7, H-2, H-4, H-5, H-7, M-7B, M-7C, M-7D, and M-7E.

(1) The uses permitted in Planning Areas B-1, B-2, B-5, B-6, C-2, C-4, C-8, F-3, G-7, H-2, H-4, H-5, H-7, M-7B, M-7C, M-7D, and M-7E of Specific Plan No. 303 shall be the same as those uses permitted in Article VII, Section 7.1 of Ordinance No. 348, except that the uses permitted pursuant to Section 7.1.a.(3); Section 7.1.b(9); and 7.1.c(1) shall not be permitted. In addition, the permitted uses identified under Section 7.1.b. shall include two family dwellings developed pursuant to Subsections AA. through FF. of this section; community centers, lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water and non-commercial fishing; water wells and appurtenant facilities; and when the gross area of a lot is twenty (20) acres or greater, the uses identified under Article XIII, Section 13.1.b. of Ordinance No. 348 shall also be included.

(2) The development standards for Planning Areas B-1, B-2, B-5, B-6, C-2, C-4, C-8, F-3, G-7, H-2, H-4, H-5, H-7, M-7B, M-7C, M-7D, and M-7E of Specific Plan No. 303 shall be the same as those standards identified in Article VII, Sections 7.2 through 7.11, except that the development standards set forth in Sections 7.3, 7.5, 7.6, and 7.11 shall be deleted and replaced by the following:

A. Lot area shall be not less than five thousand (5,000) square feet, unless cluster development subject to the development standards set forth in subsections AA. through FF. of this section is utilized. The minimum lot area shall be determined by excluding that portion of a lot that is used solely for access to the portion of a lot used as building site.

1                   B.       The minimum average width of that portion of a lot to be used as a  
2 building site shall be fifty feet (50'), with a minimum average depth of one hundred feet  
3 (100') unless cluster development subject to the development standards set forth in  
4 subsections AA. through FF. of this section is utilized. "Flag" lots shall not be permitted.

5                   C.       The minimum frontage of a lot shall be fifty (50') except that lots fronting  
6 on knuckles or cul-de-sacs may have a minimum frontage of thirty feet (30') unless cluster  
7 development subject to the development standards set forth in subsections AA. through FF.  
8 of this section is utilized. Lot frontage along curvilinear streets may be measured at the  
9 building setback in accordance with zone development standards.

10                  D.       Side yards on interior and through lots shall be not less than ten percent  
11 (10%) of the width of the lot, but not less than three feet (3') in width in any event, and  
12 need not exceed a width of five feet (5') unless cluster development subject to the  
13 development standards set forth in subsections AA. through FF. of this section is utilized.  
14 A zero lot line design may be used, in which event the alternate side yard shall be not less  
15 than ten feet (10') in width. Side yards on corner and reverse corner lots shall be not less  
16 than ten feet (10') from the existing street line or from any future street line as shown on  
17 any Specific Plan of Highways, whichever is nearer the proposed structure, upon which the  
18 main building sides, unless cluster development subject to the development standards set  
19 forth in subsections AA. through FF. of this section is utilized.

20                  E.       The rear yard shall not be less than ten feet (10') unless cluster  
21 development subject to the development standards set forth in subsections AA. through FF.  
22 of this section is utilized.

23                  In addition, when a cluster development design is utilized, for either single family or two  
24 family dwellings, the following development standards shall be applicable:

25                  AA.       The minimum overall area for each single-family dwelling unit or  
26 each individual unit within a two-family dwelling, exclusive of the area set aside  
27 for street rights of way shall be four thousand (4,000) square feet.  
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BB. The minimum lot area for individual single-family and two-family lots used as a residential building site shall be four thousand (4,000) square feet. The minimum lot area shall be determined by excluding that portion of a lot that is used solely for access to the portion of a lot used as a building site. For each dwelling unit, common open space shall be provided equal to the difference between the single-family or two-family lot area and five thousand (5,000) square feet for each single-family dwelling or ten thousand five hundred (10,500) square feet for each two-family dwelling.

CC. The minimum average width of that portion of a lot to be used as a building site shall be fifty feet (50'), with a minimum average depth of ninety feet (90'). "Flag" lots shall not be permitted.

DD. The minimum frontage of a lot shall be fifty feet (50'), except that lots fronting on knuckles or culs-de-sac may have a minimum frontage of thirty feet (30'). Lot frontage along curvilinear streets may be measured at the building setback in accordance with zone development standards.

EE. Side yards for single-family dwellings on interior and through lots shall be not less than ten percent (10%) of the width of the lot, but not less than three feet (3') in width in any event, and need not exceed a width of five feet (5'). Side yards for single-family dwellings on corner and reverse corner lots shall be not less than ten feet (10') from the existing street line or from any future street line as shown on any Specific Plan of Highways, whichever is nearer the proposed structure, upon which the main building sides. Where a zero lot line design is utilized for single-family dwellings, the alternate side yard shall be not less than ten feet (10') in width. Side yards for two-family dwellings on interior and through lots shall be not less than five feet (5') for one-story buildings; not less than ten feet (10') for two-story buildings; and not less than fifteen feet (15') for three-story buildings. Side yards for two-family dwellings on corner and reverse corner lots shall be measured from the existing street line or from any future street line as

1 shown on any Specific Plan of Highways, whichever is nearer the proposed  
2 structure, upon which the main building sides.

3 FF. The rear yard for single-family dwellings shall be not less than ten  
4 feet (10'). The rear yard for two-family dwellings shall be not less than ten feet  
5 (10') for one-story buildings, not less than fifteen feet (15') for two-story buildings;  
6 and not less than twenty feet (20') for three-story buildings.

7 (3) Except as provided above, all other zoning requirements shall be the same as  
8 those requirements identified in Article VII of Ordinance No. 348.

9 f. Planning Areas A-5, G-1 and F-4.

10 (1) The uses permitted in Planning Areas A-5, G-1 and F-4 of Specific Plan No. 303  
11 shall be the same as those permitted in Article IX, Section 9.1 of Ordinance No. 348, except that  
12 the uses permitted pursuant to Sections 9.1.a. (29), (51) and (93), b.(11)a., (12), (18), (19), and  
13 (20), d.(2), (3), (4), (5), (6), (9), (10), (11), (12) and (13), shall not be permitted.

14 In addition, the permitted uses identified under Section 9.1.a. shall include public parks and  
15 public playgrounds; golf courses; country clubs; animal hospitals with all kennels entirely indoors;  
16 health clubs; computer sales and repair stores; parcel delivery services; libraries; religious  
17 institutions; community centers; schools; and water wells and appurtenant facilities.

18 In addition, when the gross area of a lot is twenty (20) acres or greater, the permitted uses  
19 identified under Section 9.1.b. shall include the uses permitted under Article XIII, Section 13.1.b.  
20 of Ordinance No. 348.

21 In addition, the permitted uses identified under Section 9.1.d. shall include electric vehicle  
22 charging stations.

23 (2) The development standards for Planning Areas A-5, G-1 and F-4 of Specific Plan  
24 No. 303 shall be the same as those standards identified in Article IX, Section 9.4 of Ordinance No.  
25 348.

26 (3) Except as provided above, all other zoning requirements shall be the same as those  
27 requirements identified in Article IX of Ordinance No. 348.  
28

1 g. Planning Areas B-3, C-1, F-2, G-5, G-10, G-11, G-13, H-9, I-4, I-5, I-6, I-7, I-8, I-9, I-10,  
2 I-11, J-1, J-4, M-1A, M-1B, M-1C, M-5A, M-5B, M-6A, M-6B, and M-7A.

3 (1) The uses permitted in Planning Areas B-3, C-1, F-2, G-5, G-10, G-11, G-13, H-9,  
4 I-4, I-5, I-6, I-7, I-8, I-9, I-10, I-11, J-1, J-4, M-1A, M-1B, M-1C, M-5A, M-5B, M-6A, M-6B, and  
5 M-7A of Specific Plan No. 303 shall be the same as those uses permitted in Article VI, Section 6.1  
6 of Ordinance No. 348, except that the uses permitted pursuant to Section 6.1.b(5); and Section  
7 6.1.c(1). shall not be permitted. In addition, the permitted uses identified under Section 6.1.a. shall  
8 include two family dwellings developed pursuant to subsection AA. through GG. of this section;  
9 lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable  
10 irrigation water; water wells and appurtenant facilities; and when the gross acre of a lot is twenty  
11 (20) acres or greater, the uses identified under Article XIII, Section 13.1.a. and b. of Ordinance No.  
12 348 shall also be included, except that the uses permitted pursuant to Section 13.1.a(15) shall not  
13 be permitted.

14 In addition the permitted uses identified under Section 6.1.b. shall include day care centers;  
15 libraries; religious institutions; community centers; and schools.

16 (2) The development standards for Planning Areas B-3, C-1, F-2, G-5, G-10, G-11, G-  
17 13, H-9, I-4, I-5, I-6, I-7, I-8, I-9, I-10, I-11, J-1, J-4, M-1A, M-1B, M-1C, M-5A, M-5B, M-6A, M-  
18 6B, and M-7A of Specific Plan No. 303 shall be the same as those standards identified in Article  
19 VI, Section 6.2, except that the development standards set forth in Article VI, Section 6.2.b, c., d.,  
20 e.(2), e.(3) and g shall be deleted and replaced by the following:

21 A. Lot area shall be not less than six thousand (6,000) square feet, unless  
22 cluster development subject to the development standards set forth in subsection AA.  
23 through GG. of this section is utilized. The minimum lot area shall be determined by  
24 excluding that portion of a lot that is used solely for access to the portion of a lot used as  
25 building site.

26 B. The minimum average width of that portion of a lot to be used as a  
27 building site shall be sixty feet (60'), with a minimum average depth of one hundred feet  
28

1 (100') unless cluster development subject to the development standards set forth in  
2 subsections AA. through GG. of this section is utilized. "Flag" lots shall not be permitted.

3 C. The minimum frontage of a lot shall be sixty feet (60') except that lots  
4 fronting on knuckles or cul-de-sacs may have a minimum frontage of thirty-five feet (35')  
5 unless cluster development subject to the development standards set forth in subsections  
6 AA. through GG. of this section is utilized. Lot frontage along curvilinear streets may be  
7 measured at the building setback in accordance with zone development standards.

8 D. Side yards on interior and through lots shall be not less than ten percent  
9 (10%) of the width of the lot, but not less than three feet (3') in width in any event, and  
10 need not exceed a width of five feet (5') unless cluster development subject to the  
11 development standards set forth in subsection AA. through GG. of this section is utilized.  
12 Side yards on corner and reverse corner lots shall be not less than ten feet (10') from the  
13 existing street line or from any future street line as shown on any Specific Plan of  
14 Highways, whichever is nearer the proposed structure, upon which the main building sides  
15 unless cluster development subject to the development standards set forth in subsections  
16 AA. through GG. of this section is utilized.

17 E. The rear yard shall not be less than ten feet (10') unless cluster  
18 development subject to the development standards set forth in subsections AA. through  
19 GG. of this section is utilized.

20 In addition, when a cluster development design is utilized for single family or two  
21 family dwellings, the following development standards shall be applicable:

22 AA. The minimum overall area for each single-family dwelling unit or  
23 each individual unit within a two-family dwelling, exclusive of the area set aside  
24 for street rights of way shall be five thousand (5,000) square feet.

25 BB. The minimum lot area for individual single-family lots used as a  
26 residential building site shall be five thousand (5,000) square feet. The minimum  
27 lot area for two-family lots shall be five thousand (5,500) square feet. The  
28 minimum lot area shall be determined by excluding that portion of a lot that is used

1 solely for access to the portion of a lot used as a building site. For each dwelling  
2 unit, common open space shall be provided equal to the difference between the  
3 single-family or two-family lot area and six thousand (6,000) square feet for each  
4 single-family dwelling or twelve thousand (12,000) square feet for each two-  
5 family dwelling.

6 CC. The minimum average width of that portion of a lot to be used as a  
7 building site shall be fifty-five feet (55'), with a minimum average depth of one  
8 hundred feet (100'). "Flag" lots shall not be permitted.

9 DD. The minimum frontage of a lot shall be fifty-five feet (55'), except  
10 that lots fronting on knuckles or cul-de-sac may have a minimum frontage of thirty  
11 feet (30'). Lot frontage along curvilinear streets may be measured at the building  
12 setback in accordance with zone development standards.

13 EE. Side yards on interior and through lots shall be not less than ten  
14 percent (10%) of the width of the lot, but not less than three feet (3') in width in  
15 any event, and need not exceed a width of five feet (5'). Side yards on corner and  
16 reverse corner lots shall be not less than ten (10') from the existing street line or  
17 from any future street line as shown on any Specific Plan of Highways, whichever  
18 is nearer the proposed structure, upon which the main building sides. Where a zero  
19 lot line design is utilized the alternate side yard shall be not less than ten feet (10')  
20 in width.

21 FF. The rear yard for single-family dwellings shall be not less than ten  
22 feet (10'). The rear yard for two-family dwellings shall be not less than ten feet  
23 (10') for one-story buildings, not less than fifteen feet (15') for two-story buildings,  
24 and not less than twenty feet (20') for three-story buildings.

25 GG. In no case shall more than sixty percent (60%) of any lot be  
26 covered by buildings or structures.

27 (3) Except as provided above, all other zoning requirements shall be the same as those  
28 requirements identified in Article VI of Ordinance No. 348.

1           h.       Planning Area A-4

2                   (1)       The uses permitted in Planning Area A-4 of Specific Plan 303 shall be the same as  
3 those uses permitted in Article XII, Section 12.2 of Ordinance No. 348, except that the uses  
4 permitted pursuant to Section 12.2.c. (3), (7), (11) and (12); Section 12.2.d.; Section 12.2.e., 12.2.f.  
5 and 12.2.g. shall not be permitted. In addition, the permitted uses identified under Section 12.2.b.  
6 shall include water wells and appurtenant facilities, facilities related to large scale recreational uses  
7 such as golf courses and a motor sports race track and facilities related thereto, including but not  
8 limited to race track, private garages, clubhouse, tuning shop, observation tower, museum, vehicle  
9 display areas, underground fuel storage and ancillary uses in support thereof; and when the gross  
10 acre of a lot is twenty (20) acres or greater, the uses identified under Article XIII, Section 13.1.a.  
11 and b. of Ordinance No. 348 shall also be included, except that the uses permitted pursuant to  
12 Section 13.1.a.(15) shall not be permitted.

13                   In addition, the permitted uses identified under Section 12.2.b shall include aerial services  
14 including advertising, photography and tours; aerospace/aeronautical museums; aircraft taxiways;  
15 catering services/flight kitchens; conference facilities; golf courses and appurtenant facilities;  
16 convenience stores; dry cleaners; flight schools; hospitals; hotels and motels; intermodal cargo  
17 transfer terminals; research and development facilities for biomedical, chemical, electronic,  
18 mechanical and other scientific purposes; paper recycling facilities; parcel delivery services; and  
19 rental car agencies including the storage of rental cars.

20                   In addition, the permitted uses identified under Section 12.2.c. shall include cogeneration  
21 plants; structures and facilities necessary and incidental to the development, generation and  
22 transmission of electric power and gas such as power plants, booster or conversion plants,  
23 transmission lines, pipelines and the like; and incarceration and detention facilities.

24                   (2)       The development standards for Planning Area A-4 of Specific Plan No. 303 shall  
25 be the same as those standards identified in Article XII, Section 12.4 of Ordinance No.348,  
26 provided however that Article XII, Section 12,4(b)(3) shall apply only to setbacks calculated from  
27 public streets. Article XII, Section 12.4.a. is modified to provide that the minimum lot area shall  
28 be seven thousand (7,000) square feet with no minimum average width. There shall be no



1 minimum setback from any private street. Article XII, Section 12.4c.(2) is modified to provide that  
2 an observation tower built within Planning Areas A-4 and built as part of a large scale recreational  
3 use shall not exceed 70 feet in height and sports lighting, consisting of exterior nighttime lighting  
4 for ballfields, racetracks and other sporting activities, shall not be permitted.

5 (3) Except as provided above, all other zoning requirements shall be the same as  
6 those requirements identified in Article XII of Ordinance No.348.

7 i. Planning Area A-6, E-2, and E-4

8 (1) The uses permitted in Planning Areas A-6 and E-2 of Specific Plan 303 shall be  
9 the same as those uses permitted in Article IX, Section 9.1 and Article XII, Section 12.2 of  
10 Ordinance No. 348, except that the uses permitted pursuant to Article IX, Sections 9.1.a. (17), (23),  
11 (25), (27), (29), (32), (42), (51), (52), (61), (65), (67), (73), (83), (93) and (96-within Airport Land  
12 Use Compatibility Zone C); Sections 9.1.b. (7), (9), (10), (11.a.) (11.b), (11.c), (13), (14), (15),  
13 (16), (18), (19), and (20); and Sections 9.1.d. (2), (4), (5), (6), (9), (10), (11) (12), (13), and (16)  
14 and to Article XII, Section 12.2.c. (3), (7), (11) and (12); Section 12.2.d.; Section 12.2.e., 12.2.f.  
15 and 12.2.g. shall not be permitted; and uses permitted pursuant to Article IX, Section 9.1.a (35)  
16 shall not be permitted in Planning Areas E-2 and E-4.

17 In addition, the permitted uses identified under Section 9.1.a and 12.2b shall include aerial  
18 services including advertising, photography and tours; aerospace/aeronautical museums; aircraft  
19 equipment sales, service, and repair; aircraft taxiways; aviation equipment assembly; catering  
20 services/flight kitchens; conference facilities; computer and office equipment sales, service, repair  
21 and assembly; conference facilities; contractor storage yards; convenience stores; country clubs;  
22 dry cleaners; emergency and urgent care medical facilities; facilities related to large scale  
23 recreational uses such as a motor sports race track and facilities related thereto, including but not  
24 limited to race track, private garages, clubhouse, tuning shop, observation tower, museum, vehicle  
25 display areas, underground fuel storage and ancillary uses in support thereof; flight schools;  
26 ~~hospitals~~; ice houses; intermodal cargo transfer terminals; ~~libraries~~; manufacture of dairy products,  
27 not including dairies; manufacture of grain and bakery products; manufacture and repair of jewelry;  
28 manufacture of wearing apparel and accessories; manufacture and repair of measuring devices,

1 watches, clocks and related items; manufacture and repair of optical goods, medical instruments,  
2 supplies and equipment, engineering, survey and drafting instruments and photography equipment;  
3 manufacture of furniture and fixtures, including cabinets, partitions and similar small items;  
4 manufacture of cutlery, tableware, hand tools and hardware; manufacture of plumbing and heating  
5 items; manufacture of office and computing machines; manufacture, assembly, testing and repair of  
6 components, devices, equipment and systems of an electrical, electronic, or electro-mechanical  
7 nature; manufacture of non-alcoholic beverages; manufacture of confectionery products;  
8 manufacture and repair of refrigeration and heating equipment; outdoor film studio; paper  
9 recycling facilities; parcel delivery services; public parks and public playgrounds; ~~religious~~  
10 ~~institutions~~; rental car agencies including the storage of rental cars; research and development  
11 facilities for biomedical, chemical, electronic, mechanical and other scientific purposes; research  
12 and development facilities for precision components and products; and water wells and appurtenant  
13 facilities

14 In addition, the permitted uses identified under Section 9.1.a and 12.2b for Planning Area  
15 E-2 shall include racetrack recreational units.

16 In addition, the permitted uses identified under Section 9.1.d. and Section 12.2.c shall  
17 include above ground natural gas storage less than 6,000 gallons; breweries, distilleries and  
18 wineries; community centers; research and manufacture of drugs and pharmaceuticals; and paper  
19 storage and recycling within a building.

20 (2) The development standards for Planning Areas A-6 and E-2 of Specific  
21 Plan No. 303 shall be the same as those standards identified in Article XII, Section 12.4 of  
22 Ordinance No. 348. Provided however that Article XII, Section 12.4(A) is modified to provide that  
23 the minimum lot area shall be seven thousand (7,000) square feet with no minimum average width.  
24 There shall be no setback from any private street. Article XII, Section 12.4(B)(3) shall apply only  
25 to setbacks from calculated public streets. Article XII, Section 12.4(C)(2) is modified to provide  
26 that an observation tower built within Planning Areas A-6 and E-2 and built as part of a large scale  
27 recreational use shall not exceed 70 feet in height. Article XII, Section 12.4(K) is modified to  
28

1 provide that sports lighting, consisting of exterior nighttime lighting for ballfields, racetracks and  
2 other sporting activities, shall not be permitted.

3 (3) A maximum of 110 racetrack recreational units in Planning Area E-2 shall be permitted.

4 The following development standards shall apply to the recreational racetrack units:

- 5 a) Minimum lot sizes of 7,000 square feet.
- 6 b) No minimum setback is required from any private street.
- 7 c) Front yard setback shall be a minimum of thirty feet (30').
- 8 d) There shall be no rear yard setback.
- 9 e) Side yard setback shall be a minimum of five feet (5').
- 10 f) No buffer is required for uses contiguous to this Planning Area.
- 11 g) Structures shall provide vehicle storage area on the ground floor of structure.
- 12 h) Access shall be from common access areas or corridors
- 13 i) First floor of structure may include any of the following: cars, lifts, work areas,  
14 storage, mechanical, main entry, half baths, bath with locker rooms, washer/dryer  
15 area, elevator, stairs, media room, kitchenette area,
- 16 j) Second floor of structure may include any of the following: elevator, stairs,  
17 storage, mechanical, bedrooms and closets, bath area, laundry area, family  
18 room/game room, kitchen, outside patios, outside planters.
- 19 k) Third floor of structure may include any of the following: bedrooms and closets,  
20 bath, elevator, stairs, laundry area, kitchen, dining area, great room, outside patios,  
21 mechanical, storage, game room, barbeque area, outside pools and hot tubs, outside  
22 planters.
- 23 l) Overnight accommodations shall be limited to forty-five (45) consecutive nights  
24 my any owner, visitor, or any occupant including but not limited to any vehicle  
25 maintenance staff, housekeeping staff, or any form of groundskeeper. The Home  
26 Owner Association shall be responsible for the enforcement of this provision.

27 (34) Except as provided above, all other zoning requirements shall be the same as those  
28 requirements identified in Article IX and Article XII of Ordinance No. 348.

1 (45) Except as provided above, all other zoning requirements shall be the same as  
2 those requirements identified in Article IX and Article XII of Ordinance No.348.

3 j. Planning Area E-6

4 (1) The uses permitted in Planning Area E-6 of Specific Plan 303 shall be the same as  
5 those uses permitted in Article IX, Section 9.1 and Article XII, Section 12.2 of Ordinance No. 348,  
6 except that the uses permitted pursuant except that the uses permitted pursuant to Article IX,  
7 Sections 9.1.a. (17), (23), (25), (27), (29), (32), (42), (51), (52), (61), (65), (67), (73), (83), and  
8 (93); Sections 9.1.b. (7), (9), (10), (11.a.) (11.b), (11.c), (13), (14), (15), (16), (18), (19), and (20);  
9 and Sections 9.1.d. (2), (4), (5), (6), (9), (10), (11) (12), (13), and (16) and to Article XII, Section  
10 12.2.c. (3), (7), (11) and (12); Section 12.2.d.; Section 12.2.e., 12.2.f. and 12.2.g. shall not be  
11 permitted.

12 In addition, the permitted uses identified under Section 9.1.a and 12.2b shall include aerial  
13 services including advertising, photography and tours; aerospace/aeronautical museums; aircraft  
14 equipment sales, service, and repair; aircraft taxiways; aviation equipment assembly; catering  
15 services/flight kitchens; conference facilities; computer and office equipment sales, service, repair  
16 and assembly; conference facilities; contractor storage yards; convenience stores; country clubs;  
17 dry cleaners; emergency and urgent care medical facilities; facilities related to large scale  
18 recreational uses such as a motor sports race track and facilities related thereto, including but not  
19 limited to race track, private garages, single family residential including duplex units defined as a  
20 structure with two dwelling units placed beside one another sharing a common wall, clubhouse,  
21 tuning shop, observation tower, museum, vehicle display areas, underground fuel storage and  
22 ancillary uses in support thereof; flight schools; health and exercise centers; hospitals; ice houses;  
23 intermodal cargo transfer terminals; libraries; manufacture of dairy products, not including dairies;  
24 manufacture of grain and bakery products; manufacture and repair of jewelry; manufacture of  
25 wearing apparel and accessories; manufacture and repair of measuring devices, watches, clocks and  
26 related items; manufacture and repair of optical goods, medical instruments, supplies and  
27 equipment, engineering, survey and drafting instruments and photography equipment; manufacture  
28 of furniture and fixtures, including cabinets, partitions and similar small items; manufacture of

1 cutlery, tableware, hand tools and hardware; manufacture of plumbing and heating items;  
2 manufacture of office and computing machines; manufacture, assembly, testing and repair of  
3 components, devices, equipment and systems of an electrical, electronic, or electro-mechanical  
4 nature; manufacture of non-alcoholic beverages; manufacture of confectionery products;  
5 manufacture and repair of refrigeration and heating equipment; outdoor film studio; paper  
6 recycling facilities; parcel delivery services; public parks and public playgrounds; religious  
7 institutions; rental car agencies including the storage of rental cars; research and development  
8 facilities for biomedical, chemical, electronic, mechanical and other scientific purposes; research  
9 and development facilities for precision components and products; and water wells and appurtenant  
10 facilities.

11 In addition, the permitted uses identified under Section 9.1.d. and Section 12.2.c shall  
12 include above ground natural gas storage less than 6,000 gallons; breweries, distilleries and  
13 wineries; community centers; research and manufacture of drugs and pharmaceuticals; and paper  
14 storage and recycling within a building.

15 (2) The development standards for Planning Area E-6 of Specific Plan No. 303 shall  
16 be the same as those standards identified in Article XII, Section 12.4 and Article XVIII, Section  
17 18.5 of Ordinance No. 348 except those development standards set forth in Article XVIII, Section  
18 18.5 a, b, c, d, e, f, g, h, i, j, and k, shall be deleted and replace by the following:

19 a. If residential uses are located contiguous to nonresidential uses, then the following  
20 standards shall apply:

- 21 1. Minimum lot area shall be seven thousand (7,000) square feet with no  
22 minimum average width.
- 23 2. There shall be no minimum setback from any private street.
- 24 3. Standard Setbacks
  - 25 i. No minimum setback is required from any private street.
  - 26 ii. Front Yard: 30-foot minimum
  - 27 iii. Rear Yard: No minimum
  - 28 iv. Side Yard: 5-feet.

- 1                                   4. Access to residential structures shall be from common access areas or
- 2                                   corridors.
- 3                                   5. Residential structures must provide vehicle storage area on the ground floor
- 4                                   of a structure.
- 5                                   6. Up to a maximum of 120 units in Planning Area E-6 shall be permitted.

6                                   And provided however, that Article XII, Section 12.4(A) is modified to provide that the minimum  
7                                   lot area shall be seven thousand (7,000) square feet with no minimum average width. There shall  
8                                   be no setback from any private street. Article XII, Section 12.4(B)(1) is modified that there is no  
9                                   minimum setback. Article XII, Section 12.4(B)(3) shall apply only to setbacks from calculated  
10                                   public streets. Article XII, Section 12.4(K) is modified to provide that sports lighting, consisting of  
11                                   exterior nighttime lighting for ballfields, racetracks and other sporting activities, shall not be  
12                                   permitted.

13                                   (3)       Except as provided above, all other zoning requirements shall be the same as those  
14                                   requirements identified in Article IX and Article XII of Ordinance No. 348.

15                                   (4)       Except as provided above, all other zoning requirements shall be the same as  
16                                   those requirements identified in Article IX and Article XII of Ordinance No.348.

17                                   k.        Planning Areas E-5, E-7, and E-8

18                                   (1)       The uses permitted in Planning Areas E-5, E-7 and E-8 of Specific Plan 303 shall  
19                                   be the same as those uses permitted in Article IX, Section 9.1 and Article XII, Section 12.2 of  
20                                   Ordinance No. 348, except that the uses permitted pursuant except that the uses permitted pursuant  
21                                   to Article IX, Sections 9.1.a. (17), (23), (25), (27), (29), (32), (35), (42), (51), (52), (61), (65), (67),  
22                                   (73), (83), (93), and (96); Sections 9.1.b. (7), (9), (10), (11.a.) (11.b), (11.c), (13), (14), (15), (16),  
23                                   (18), (19), and (20); and Sections 9.1.d. (2), (4), (5), (6), (9), (10), (11) (12), (13), and (16) and to  
24                                   Article XII, Section 12.2.c. (3), (7), (11) and (12); Section 12.2.d.; Section 12.2.e., 12.2.f. and  
25                                   12.2.g. shall not be permitted.

26                                   In addition, the permitted uses identified under Section 9.1.a and 12.2b shall include aerial  
27                                   services including advertising, photography and tours; aerospace/aeronautical museums; aircraft  
28                                   equipment sales, service, and repair; aircraft taxiways; aviation equipment assembly; catering

1 services/flight kitchens; conference facilities; computer and office equipment sales, service, repair  
2 and assembly; conference facilities; contractor storage yards; convenience stores; country clubs;  
3 dry cleaners; emergency and urgent care medical facilities; facilities related to large scale  
4 recreational uses such as a motor sports race track and facilities related thereto, including but not  
5 limited to race track, private garages, single family residential including duplex units defined as a  
6 structure with two dwelling units placed beside one another sharing a common wall, clubhouse,  
7 tuning shop, observation tower, museum, vehicle display areas, underground fuel storage and  
8 ancillary uses in support thereof; flight schools; health and exercise centers; hospitals; ice houses;  
9 intermodal cargo transfer terminals; libraries; manufacture of dairy products, not including dairies;  
10 manufacture of grain and bakery products; manufacture and repair of jewelry; manufacture of  
11 wearing apparel and accessories; manufacture and repair of measuring devices, watches, clocks and  
12 related items; manufacture and repair of optical goods, medical instruments, supplies and  
13 equipment, engineering, survey and drafting instruments and photography equipment; manufacture  
14 of furniture and fixtures, including cabinets, partitions and similar small items; manufacture of  
15 cutlery, tableware, hand tools and hardware; manufacture of plumbing and heating items;  
16 manufacture of office and computing machines; manufacture, assembly, testing and repair of  
17 components, devices, equipment and systems of an electrical, electronic, or electro-mechanical  
18 nature; manufacture of non-alcoholic beverages; manufacture of confectionery products;  
19 manufacture and repair of refrigeration and heating equipment; outdoor film studio; paper  
20 recycling facilities; parcel delivery services; religious institutions; rental car agencies including the  
21 storage of rental cars; research and development facilities for biomedical, chemical, electronic,  
22 mechanical and other scientific purposes; research and development facilities for precision  
23 components and products; and water wells and appurtenant facilities.

24 In addition, the permitted uses identified under Section 9.1.d. and Section 12.2.c shall  
25 include above ground natural gas storage less than 6,000 gallons; breweries, distilleries and  
26 wineries; cogeneration plants; community centers; research and manufacture of drugs and  
27 pharmaceuticals; and paper storage and recycling within a building.

1                   (2)     The development standards for Planning Areas E-5, E-7, and E-8 of Specific Plan  
2     No. 303 shall be the same as those standards identified in Article XII, Section 12.4 and Article  
3     XVIII, Section 18.5 of Ordinance No. 348 except those development standards set forth in Article  
4     XVIII, Section 18.5 a, b, c, d, e, f, g, h, i, j, and k, shall be deleted and replace by the following:

5                   b.     If residential uses are located contiguous to nonresidential uses, then the following  
6     standards shall apply:

- 7                   1.     Minimum lot area shall be seven thousand (7,000) square feet with no  
8                   minimum average width.
- 9                   2.     There shall be no minimum setback from any private street.
- 10                  3.     Standard Setbacks
  - 11                   i.     No minimum setback is required from any private street.
  - 12                   ii.    Front Yard: 30-foot minimum
  - 13                   iii.   Rear Yard: No minimum
  - 14                   iv.   Side Yard: 5-feet.
- 15                  4.     Access to residential structures shall be from common access areas or  
16                   corridors.
- 17                  5.     Residential structures must provide vehicle storage area on the ground floor  
18                   of a structure.
- 19                  6.     Up to a maximum of 19 units in Planning Area E-5, a maximum of 5 units in  
20                   Planning Area E-7, and a maximum of 15 units in Planning Area E-8 shall be  
21                   permitted.

22     And provided however that Article XII, Section 12.4(A) is modified to provide that the minimum  
23     lot area shall be seven thousand (7,000) square feet with no minimum average width and there shall  
24     be no setback from any private street. Article XII, Section 12.4(B)(1) is modified that there is no  
25     minimum setback. Article XII, Section 12.4(B)(3) shall apply only to setbacks from calculated  
26     public streets. Article XII, Section 12.4(K) is modified to provide that sports lighting, consisting of  
27     exterior nighttime lighting for ballfields, racetracks and other sporting activities, shall not be  
28     permitted.



1 (3) Except as provided above, all other zoning requirements shall be the same as those  
2 requirements identified in Article IX and Article XII of Ordinance No. 348.

3 (4) Except as provided above, all other zoning requirements shall be the same as  
4 those requirements identified in Article IX and Article XII of Ordinance No.348.

5 1. Planning Area M-4

6 (1) The uses permitted in Planning Area M-4 of Specific Plan No. 303 shall be the  
7 same as those uses permitted in Article VI, Section 6.1 of Ordinance No. 348. In addition, the  
8 permitted uses identified under Section 6.1.a. shall include government offices, courthouses, police  
9 stations, fire stations, libraries, museums, and public schools.

10 (2) The development standards for Planning Area M-4 of Specific Plan No. 303 shall  
11 be the same as those standards identified in Article VI, Section 6.2 of Ordinance No. 348.

12 (3) Except as provided above, all other zoning requirements shall be the same as those  
13 requirements identified in Article XI of Ordinance No. 348.

14 Section 3. This ordinance shall take effect 30 days after its adoption.

15  
16 BOARD OF SUPERVISORS OF THE COUNTY  
OF RIVERSIDE, STATE OF CALIFORNIA

17  
18 By: \_\_\_\_\_  
19 Chairman

20 ATTEST:

21 NANCY ROMERO  
22 Clerk of the Board

23  
24 By: \_\_\_\_\_  
Deputy

25  
26 (SEAL)

27 APPROVED AS TO FORM:  
28 [Date]

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By: \_\_\_\_\_

KARIN WATTS-BAZAN  
Deputy County Counsel

DRAFT

### 3. SPECIFIC PLAN

The Eastern Coachella Valley has experienced significant population growth and residential development in recent years, as evidenced by the addition of 78,019 residents in the cities of Coachella, Indio, La Quinta, and Palm Desert between 2000 and 2008 (a 57 percent increase)<sup>1</sup>. In addition, the level of planning activity in the project area (e.g., Airport Master Plan) reflects the strategic location of the eastern Coachella Valley and the Kohl Ranch site with respect to the Los Angeles, Orange County, and San Diego metropolitan areas, the Inland Empire and Mexico. Access to the regional transportation network affords the site maximum potential to reach these extensive markets. The land use designations associated with the Kohl Ranch Specific Plan are intended to provide a balanced mix of land uses throughout the project site and within individual neighborhoods, and are distributed throughout the site so as to support ongoing planning efforts in the project vicinity, and to further the goals of the Coachella Valley Enterprise Zone.

The diverse business and employment opportunities, living environments, recreational and visual amenities, and roadway and other infrastructure improvements that would be planned and permitted under the Specific Plan would enhance the attractiveness of the Jacqueline Cochran Regional Airport vicinity, and would reinforce the desirability of the area for potential future development opportunities within and surrounding the airport. The proposed project would also facilitate development of the site by a single industrial user or multiple users interested in the Coachella Valley as the location for a major jobs-generating enterprise. The Specific Plan would be compatible with the policies in the Comprehensive General Plan which recognize the growth potential of the project area as a result of the airport expansion and which recommend that industrial uses be among those to locate in this area. The Specific Plan designation also would provide a mechanism for ensuring that major ongoing planning efforts for the area are properly coordinated and mutually supportive.

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<sup>1</sup> State of California, Department of Finance, *E-4 Population Estimates for Cities, Counties and the State, 2001–2009, with 2000 Benchmark*. Sacramento, California, May 2009. (Accessed on 7/17/10 at <http://www.dof.ca.gov/research/demographic/reports/estimates/e-4/2001-09/>)

## 3.1 PROJECT-WIDE PLANNING STANDARDS

The following section of the Specific Plan details the land uses and development standards for the Kohl Ranch community as a whole. Illustrations are provided to portray the various planning areas, infrastructure plans, and design treatments such as buffers, landscaping and streetscapes. The visual images and themes addressed in this section set the overall tone for the project.

Planning objectives for the Kohl Ranch Specific Plan include:

- Increased market potential and attractiveness of the Jacqueline Cochran Regional Airport and vicinity;
- Support for planning, economic development and development efforts, in accordance with the goals of the Master Plan for the expansion of Jacqueline Cochran Regional Airport;
- Flexibility to respond to changing market conditions, through designation of golf course as an alternate land use;
- A balanced, living and working environment that provides a mix of land uses including a variety of housing products and employment opportunities; and
- Cohesive, balanced neighborhoods, relating to overall project phasing, which can be developed separately, or together.

### 3.1.1 Comprehensive Land Use Plan

The Kohl Ranch Specific Plan consists of a balanced array of land uses including residential, business, commercial, industrial, open space, and public facilities. Both living and working opportunities will be available within the thirteen project neighborhoods, with the specific mix of uses dependent upon the neighborhood location, relationship to adjacent uses such as Jacqueline Cochran Regional Airport, and access to the regional transportation network. In general, the neighborhoods in the northern portion of the site have the highest concentration of employment uses, which can best maximize the advantages of proximity to the airport, and which are least likely to be affected by airport-related impacts such as noise. The land uses within these neighborhoods are designed to locate employment opportunities in close proximity to residential areas, thereby reducing the number of vehicle trips required, and to create centers of local activity that prosper from the mix of commercial, industrial, recreation, and business uses. These northernmost neighborhoods provide jobs and services to areas both on and off site.

The residential portion of the land use plan includes a target of 7,162 dwelling units but with a maximum not to exceed 7,171 dwelling units. These units are to be distributed among mixed use and four different density classifications on approximately 1,393 acres with an additional 376 acres of open space. The business, commercial, and industrial land use categories will comprise approximately 277, 28, and 81 acres, respectively. Commercial areas will serve the Kohl Ranch project as well as neighboring communities. Mixed Use and industrial uses will be oriented

toward the Jacqueline Cochran Regional Airport as well as larger regional markets, and are intended to provide employment opportunities to project area residents.

A target maximum number of dwelling units is specified for each planning area where permissible. The target is based on the applicable density range and may or may not represent the maximum number of units allowed within the density range for any single planning area. Therefore, any given implementing subdivision may increase the number of units beyond the target density up to the maximum allowed for its planning area without a specific plan amendment, provided that the total number of dwelling units within the Kohl Ranch Specific Plan does not exceed 7,171 dwelling units. Further, the number of dwelling units contained in an implementing subdivision application may exceed the maximum number of allowable units specified in the Specific Plan for any single planning area, by not more than ten percent (10%) without an amendment to this plan, provided that the maximum number of dwelling units in other planning areas are reduced by an equivalent number of units such that the total number of dwelling units within the Kohl Ranch Specific Plan does not exceed 7,171 dwelling units. See Section 3.1.11, Specific Plan Administration, for the approach to tracking units during the build out of the project. Density transfers will be required to receive administrative approval. The land uses proposed for the Kohl Ranch Specific Plan are identified in **Table 3.1-A, Land Use Plan Statistical Summary**, **Table 3.1-B, Land Use Summary by Planning Area**, and depicted in **3.1-1, Land Use Plan**.

Table 3.1-A, Land Use Plan Statistical Summary

Land Use	Acreage	Percent of Total	Percent of Developable Area <sup>1</sup>	Target Dwelling Units
<b>RESIDENTIAL</b>				
Medium Density Residential (MDR)	501.33	23.2	32.8	1,652
Medium High Density Residential (MHDR)	467.76	21.6	30.6	3,245
High Density Residential (HDR)	140.35	6.5	9.2	1,629
Very High Density Residential (VHDR)	30.85	1.4	2.0	477
<b>Total Residential</b>	<b>1,140.29</b>	<b>52.7</b>	<b>74.7</b>	<b>7,003</b>
<b>INDUSTRIAL</b>				
Heavy Industrial (HI)	81.17	3.8	5.3	
<b>Total Industrial</b>	<b>81.17</b>	<b>3.8</b>	<b>5.3</b>	
<b>BUSINESS</b>				
Mixed Use/Air Park (MU/AP)	24.45	1.1	1.6	
Mixed Use	252.73	11.7	16.6	159 <sup>3</sup>
<b>Total Business</b>	<b>277.18</b>	<b>12.8</b>	<b>18.2</b>	<b>159<sup>3</sup></b>
<b>COMMERCIAL</b>				
Commercial Retail (CR)	28.27	1.3	1.9	
<b>Total Commercial</b>	<b>28.27</b>	<b>1.3</b>	<b>1.9</b>	
<b>OTHER</b>				
Open Space (OS)	375.94	17.4		
Public Facilities/Schools (PF)	84.30	3.9		
Right-of-Way (ROW)	175.5	8.1		
<b>Total Other</b>	<b>641.44</b>	<b>29.4</b>		
<b>TOTAL</b>	<b>2,162.65</b>	<b>100%</b>	<b>100%</b>	<b>7,162</b>

1. Assumes total of 1,526.91 acres of developable land. This does not include land uses in "other" category.
2. The ROW acreage includes local streets, which are not depicted on the Land Use Plan. See **Figure 3.1-2, Circulation Plan**.
3. This number represents a maximum, rather than a target.

**Table 3.1-B, Land Use Summary By Planning Area**

Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
A-1	Open Space (OS)		0.63	
A-2	Mixed Use/Air Park (MU/AP)		24.45	
A-3	Open Space (OS)		10.77	
A-4	Heavy Industrial (HI)		81.17	
A-5	Commercial Retail (CR)		17.61	
A-6	Mixed Use (MU)		46.18	
A-7	Open Space (OS)		9.97	
B-1	Medium High Density Residential (MHDR)	210	30	5.0-8.0
B-2	Medium High Density Residential (MHDR)	177	25.24	5.0-8.0
B-3	Medium Density Residential (MDR)	117	39.01	2.0-5.0
B-5	Medium High Density Residential (MHDR)	229	32.7	5.0-8.0
B-6	Medium High Density Residential (MHDR)	183	26.18	5.0-8.0
C-1	Medium Density Residential (MDR)	122	40.77	2.0-5.0
C-2	Medium High Density Residential (MHDR)	218	31.16	5.0-8.0
C-4	Medium High Density Residential (MHDR)	349	49.83	5.0-8.0
C-5	Open Space (OS)		9.97	
C-6	Very High Density Residential (VHDR)	477	30.85	14.0-20.0
C-8	Medium High Density Residential (MHDR)	105	14.96	5.0-8.0
D-1	Open Space (OS)		110.66	
E-1	Open Space (OS)		5.61	

Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
E-2 <sup>2</sup>	Mixed Use (MU)		148.74	
E-3	Open Space (OS)		69.33	
E-4	Mixed Use (MU)		12.25	
E-5	Mixed Use (MU)	19 <sup>1</sup>	5.31	
E-6	Mixed Use (MU)	120 <sup>1</sup>	33.99	
E-7	Mixed Use (MU)	5 <sup>1</sup>	1.32	
E-8	Mixed Use (MU)	15 <sup>1</sup>	4.94	
F-2	Medium Density Residential (MDR)	158	71.64	2.0-5.0
F-3	Medium High Density Residential (MHDR)	635	79.94	5.0-8.0
F-4	Commercial Retail (CR)		6.23	
G-1	Commercial Retail (CR)		4.43	
G-2	Open Space (OS)		1.44	
G-3	Open Space (OS)		1.19	
G-4	Open Space (OS)		5.6	
G-5	Medium Density Residential (MDR)	69	34.6	2.0-5.0
G-6	Open Space (OS)		13.65	
G-7	Medium High Density Residential (MHDR)	432	60.38	5.0-8.0
G-8	High Density Residential (HDR)	282	20.6	8.0-14.0
G-9	Open Space (OS)		6.17	
G-10	Medium Density Residential (MDR)	37	18.57	2.0-5.0
G-11	Medium Density Residential (MDR)	70	26.86	2.0-5.0



Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
G-12	Open Space (OS)		5.35	
G-13	Medium Density Residential (MDR)	53	14.17	2.0-5.0
H-1	Open Space (OS)		5.12	
H-2	Medium High Density Residential (MHDR)	118	18.65	5.0-8.0
H-3	Open Space (OS)		3.79	
H-4	Medium High Density Residential (MHDR)	141	22.4	5.0-8.0
H-5	Medium High Density Residential (MHDR)	45	7.83	5.0-8.0
H-6	Open Space (OS)		11.43	
H-7	Medium High Density Residential (MHDR)	151	20.99	5.0-8.0
H-8	High Density Residential (HDR)	189	13.55	8.0-14.0
H-9	Medium Density Residential (MDR)	66	20.03	2.0-5.0
H-10	Open Space (OS)		3.96	
I-1	Open Space (OS)		9.6	
I-2	Open Space (OS)		3.9	
I-3	Open Space (OS)		0.5	
I-4	Medium Density Residential (MDR)	35	7	2.0-5.0
I-5	Medium Density Residential (MDR)	51	10.6	2.0-5.0
I-6	Medium Density Residential (MDR)	42	9.6	2.0-5.0
I-7	Medium Density Residential (MDR)	29	6.7	2.0-5.0
I-8	Medium Density Residential (MDR)	40	8	2.0-5.0
I-9	Medium Density Residential (MDR)	57	11.5	2.0-5.0

Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
I-10	Medium Density Residential (MDR)	47	12.4	2.0-5.0
I-11	Medium Density Residential (MDR)	37	10.1	2.0-5.0
J-1	Medium Density Residential (MDR)	130	37.52	2.0-5.0
J-2	Open Space/Lake (OS-LAKE)		22	
J-3	Open Space/Lake (OS)		8.8	
J-4	Medium Density Residential (MDR)	189	43.36	2.0-5.0
J-5	Open Space (OS)		1	
J-6	Open Space (OS)		2.2	
J-7	Open Space (OS)		3.3	
J-8	Open Space (OS)		1.3	
K-1	Open Space/Lake (OS-LAKE)		22.4	
K-2	Open Space/Lake (OS)		8.8	
K-3	Open Space (OS)		3.2	
L-1	High Density Residential (HDR)	1,158	106.2	8.0-14.0
L-2	Open Space (OS)		4.1	
L-3	Open Space (OS)		1.1	
M-1A	Medium Density Residential (MDR)	8	2.5	2.0-5.0
M-1B	Medium Density Residential (MDR)	68	18.9	2.0-5.0
M-1C	Medium Density Residential (MDR)	61	16.4	2.0-5.0
M-2	Open Space (OS)		1.9	
M-3	Open Space (OS)		1.6	

Planning Area	Land Use Code	Target Dwelling Units	Acres	Density Range
M-4	Public Facility (PF)		84.3	
M-5A	Medium Density Residential (MDR)	14	4.1	2.0-5.0
M-5B	Medium Density Residential (MDR)	61	15.7	2.0-5.0
M-6A	Medium Density Residential (MDR)	14	3.8	2.0-5.0
M-6B	Medium Density Residential (MDR)	57	13.4	2.0-5.0
M-7A	Medium Density Residential (MDR)	20	4.1	2.0-5.0
M-7B	Medium High Density Residential (MHDR)	56	11	5.0-8.0
M-7C	Medium High Density Residential (MHDR)	32	5.9	5.0-8.0
M-7D	Medium High Density Residential (MHDR)	82	15.1	5.0-8.0
M-7E	Medium High Density Residential (MHDR)	82	15.5	5.0-8.0
M-8	Open Space (OS)		2.9	
M-9	Open Space (OS)		2.7	
Right-of-Way			175.5	
<b>TOTALS</b>		<b><u>7.162</u></b>	<b><u>2.162.65</u></b>	

Note: Planning Area's B-4, B-7, C-3 and C-7 have been intentionally left out.

1. This number represents a maximum rather than a target.

2. This planning area allows for 110 racetrack recreational units

**Figure 3.1-1, Land Use Plan**

## Residential

Residential areas within the project will provide a target of 6,988 dwelling units but with a maximum not to exceed 7,171 dwelling units, with an overall gross density of 4.03 dwelling units per gross residential acre<sup>1</sup>. The residential element of the plan comprises 1,140.29 acres or 52.7 percent of the total project site. Four residential land uses are proposed, Medium Density Residential, Medium High Density Residential, High Density Residential and Very High Density Residential, establishing appropriate transitions to adjacent non-residential land uses, both on and off site. It should be noted that residential planning areas can be developed to a lower density than that specified by the planning area land use designation, without requiring an amendment to the Specific Plan. Additionally, the maximum density and the total number of dwelling units within the high and very density residential designations can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code. Residential uses north of Avenue 62 and west of Tyler Street are intended to house employees of the nearby commercial, business and industrial uses.

Each of the residential land use designations (i.e., Medium Density Residential, Medium High Density Residential, High Density Residential and Very High Density Residential) can be developed with several product types, including those which permit clustering of residential density within planning areas. Residential product types include: Detached Dwelling Units, Cluster Detached Dwelling Units, and Cluster Attached Dwelling Units.

The residential designations each have a unique set of development standards designed to accommodate a variety of housing product types. These different residential designations are intended to provide flexibility to develop in areas that are affected by the open space requirements and noise impacts of the Jacqueline Cochran Regional Airport safety zones and the limitations imposed by the utility easement beltway in the central portion of the site. By allowing for clustering of development within planning areas, it is possible to retain residential densities while providing adequate open space for drainage ways, recreational and other lakes and golf courses, the power line easement, and the airport zones.

Under the Specific Plan, it is possible to develop one or more of the product types within an individual planning area or neighborhood, provided that the development is consistent with the project's Design Guidelines.

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<sup>1</sup> The overall gross density for residential areas was determined by dividing the total number of dwelling units by the acreage devoted to residential land uses, open space, public facilities, and local residential streets. The gross density—if determined by dividing the total dwelling units by the residential acreage—is 6.28. This density is reduced to 3.31 if the entire site acreage is used.

**Medium Density Residential (2.0–5.0 du/acre)**

Medium Density Residential units are proposed throughout the project site. A target of 1,652 dwelling units on 501.33 acres is planned. This is based on a density range of 2.0–5.0 du/ac. The purpose of this designation is to provide a medium density residential alternative in the plan. The intent is to: 1) locate lower density land uses at the project periphery adjacent to off-site lands currently in low intensity uses such as agriculture; 2) provide a transition between open space/recreational areas and medium density land uses; 3) provide for limited residential uses where permitted within the Extended Runway Centerline (ERC) airport safety zone; and 4) create a housing opportunity offering greater private open space amenity than is available in the higher density residential zones.

In addition to One-Family Dwellings development, additional uses within the Medium Density Residential land use designation may include such uses as two-family dwellings developed as cluster development; lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water; water wells and appurtenant facilities; day care centers; libraries; religious institutions; community centers; and schools. Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

**Medium High Density Residential (5.0–8.0 du/acre)**

Medium High Density Residential units are proposed throughout the project site. A target of 3,245 dwelling units on 467.76 acres is planned. This is based on a density of range of 5.0–8.0 du/acre. The purpose of this designation is to provide a lower density creative residential alternative in the plan. The intent is to: 1) create the potential for single-family, first home buyers; 2) offer a desirable housing product for retirees and second home buyers; and 3) accommodate residents seeking more private open space amenities than are available in the High and Very High Density Residential areas.

In addition to Multiple Family Dwellings development, additional uses within the Medium High Density Residential land use designation may include such uses as dwellings developed as cluster development; lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water; water wells and appurtenant facilities; day care centers; libraries; religious institutions; community centers; and schools. Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

**High Density Residential (8.0–14.0 du/acre)**

High Density Residential land uses are located in close proximity to community amenities and trails systems for the project. A target of approximately 1,629 dwelling units on 140.35 acres is planned, assuming a density range of 8.0–14.0 du/acre. The purpose of this designation is to enhance the range of housing types provided within the project area. The intent is to: 1) achieve an affordable living environment for those working in the project vicinity; 2) provide a more compact housing product within close proximity to commercial services and employment opportunities; 3) create pedestrian-oriented communities with common open space and recreation facilities; and 4) orient residential areas around community focal points such as churches and community centers. The maximum density and the target number of dwelling units within the high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code.

In addition to Multiple Family Dwellings development, additional uses within the High Density Residential land use designation may include such uses as two-family dwellings developed as cluster development; lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water; and water wells and appurtenant facilities. Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

**Very High Density Residential (14.0–20.0 du/acre)**

Very High Density Residential land uses are located in close proximity to community commercial cores established for the project. A target of approximately 477 dwelling units on 30.85 acres is planned, assuming a density range of 14.0–20.0 du/acre. The purpose of this designation is to enhance the range of housing types provided within the project area. The intent is to: 1) achieve an affordable living environment for those working in the project vicinity; 2) provide a more compact housing product within close proximity to commercial services and employment opportunities; 3) create pedestrian-oriented communities with common open space and recreation facilities; and 4) orient residential areas around community focal points such as churches and community centers. The maximum density and the target number of dwelling units within the very high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code.

In addition to Multiple Family Dwellings development, additional uses within the Very High Density Residential land use designation may include such uses as two-family dwellings developed as cluster development; lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water; and water wells and appurtenant facilities. Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

## Industrial

One industrial land use category is proposed offering a range of employment opportunities that respond to market conditions and the skills base of the local labor force. A total of approximately 81.17 acres of heavy industrial uses are concentrated in the northern portion of the site, and in close proximity to the Jacqueline Cochran Regional Airport and Polk Street, a designated arterial highway providing access to the airport and points east.

The purpose of this designation is to allow for more intense, industrial/manufacturing uses or other uses compatible with the regional airport. The intent is to: 1) encourage land uses which provide relatively greater levels of employment than is typically associated with industrial uses such as warehousing and distribution; 2) allow for uses that require outdoor storage; and 3) maximize use of the regional transportation system, including air, rail and the road network.

In addition to Manufacturing-Heavy development, additional uses within the Heavy Industrial land use designation may include such uses as water wells and appurtenant facilities; aerial services including advertising, photography and tours; aerospace/aeronautical or automotive museums; aircraft taxiways; catering services/flight kitchens; conference facilities; golf courses and appurtenant facilities; convenience stores; dry cleaners; flight schools; hospitals; hotels and motels; intermodal cargo transfer terminals; research and development facilities for biomedical, chemical, electronic, mechanical and other scientific purposes; paper recycling facilities; parcel delivery services; and rental car agencies including the storage of rental cars; motor sports race track and facilities necessary and incidental thereto including, a race track, private garages, fuel storage, buildings for vehicle display, tuning shop, and observation tower; cogeneration plants; structures and facilities necessary and incidental to the development, generation and transmission of electric power and gas such as power plants, booster or conversion plants, transmission lines, pipelines and the like; and incarceration and detention facilities. Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.



## Business

### *Mixed Use/Air Park*

The Mixed Use/Air Park designation is located in the northernmost portion of the project site, just south of the Jacqueline Cochran Regional Airport. Approximately 24.45 acres are devoted to this land use. The purpose of this designation is to accommodate a wide range of land uses including office, service, commercial, airport-related, incubator business, and very light industrial. The intent is to: 1) complement expansion plans for the Jacqueline Cochran Regional Airport by providing needed services and commercial uses located near the Tyler Street airport entrance; 2) create a supportive environment for start-up businesses; 3) devise standards that encourage combinations of uses such as office/sales/storage/assembly; and 4) provide support services for business and industrial uses in the project area.

In addition to General Commercial development, additional uses within the Mixed Use/Air Park land use designation may include such uses as aviation equipment assembly; communication equipment and microwave sales and installation; computer and office equipment sales, service, repair and assembly; conference facilities; country clubs, manufacture of dairy products, not including dairies; emergency and urgent care medical facilities; libraries; manufacture of grain and bakery products; health and exercise centers; hospitals; ice houses; jewelry manufacture and repair; manufacture of wearing apparel and accessories; manufacture and repair of measuring devices, watches, clocks and related items; manufacture and repair of optical goods, medical instruments, supplies and equipment, engineering, survey and drafting instruments and photography equipment; manufacture of handbags, luggage, footwear, and other personal leather goods; manufacture of cutlery, tableware, hand tools and hardware; manufacture of plumbing and heating items; vehicle storage and impoundment; manufacture of office and computing machines; manufacture, assembly, testing and repair of components, devices, equipment and systems of an electrical, electronic, or electro-mechanical nature; manufacture of non-alcoholic beverages; manufacture of confectionery products; manufacture and repair of refrigeration and heating equipment; printing of periodicals, books, forms, cards, and similar items; public parks and public playgrounds; golf courses; religious institutions; facilities for research and development of precision components and products; facilities related to alternative energy development; large-scale recreational uses such as a motor sports race track and facilities related thereto, including but not limited to race track, private garages, clubhouse, tuning shop, observation tower, museum, vehicle display areas, and ancillary uses in support thereof; water wells and appurtenant facilities; aerial service businesses including advertising, photography and tours; aerospace/aeronautical museums; aircraft equipment sales, service and repair; contractor storage yards; flight schools; intermodal cargo transfer facilities; manufacture of furniture and fixtures, including cabinets, partitions, and similar small items; manufacture of bicycles; parcel delivery services; warehousing and distribution; community centers; schools; meat and poultry processing not including slaughtering or rendering of animals; paper shredding facilities; research and manufacture of drugs and pharmaceuticals; manufacture of soaps, cleaners and toiletries; wrought iron fabrication; machine, welding and blacksmith shops; breweries, distilleries and wineries; paper storage and recycling within a building; recycling processing facilities; paper and paperboard mills; manufacture of containers and boxes; and above ground natural gas storage. Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance an approved list of allowable uses.

**Mixed Use**

The Mixed Use designation is located in the northernmost portion of the project site, just south of the Jacqueline Cochran Regional Airport. Approximately 252.73 acres are devoted to this land use. The purpose of this designation is to accommodate a wide range of land uses including office, service, commercial, airport-related, incubator business, mixed-use residential units, very light industrial, and large scale recreational development. The intent is to: 1) complement expansion plans for the Jacqueline Cochran Regional Airport by providing needed services and commercial uses located near the Tyler Street airport entrance; 2) devise standards that encourage combinations of uses such as office/sales/storage/assembly; 3) provide support services for business and industrial uses in the project area; and 4) provide standards unique to development of large scale recreational use.

In addition to General Commercial and Manufacturing-Heavy development, additional uses within the Mixed Use/Air Park land use designation may include such uses as above ground natural gas storage less than 6,000 gallons; aerial service businesses including advertising, photography and tours; aircraft equipment sales, service and repair; contractor storage yards; aircraft taxiways; aviation equipment assembly; breweries, distilleries and wineries; catering services/flight kitchens; community centers; computer and office equipment sales, service, repair and assembly; conference facilities; convenience stores; country clubs; dry cleaners; emergency and urgent care medical facilities; facilities for research and development of precision components and products; flight schools; hospitals; hotels and motels; ice houses; intermodal cargo transfer facilities; jewelry manufacture and repair; facilities related to large scale recreational uses such as a motor sports race track and facilities related thereto, including but not limited to race track, private garages, single-family residential including duplex units defined as a structure with two dwelling units placed beside one another sharing a common wall, [racetrack recreational units](#), clubhouse, tuning shop, observation tower, museum, vehicle display areas, underground fuel storage and ancillary uses in support thereof; libraries; manufacture and repair of measuring devices, watches, clocks and related items; manufacture and repair of optical goods, medical instruments, supplies and equipment, engineering, survey and drafting instruments and photography equipment; manufacture and repair of refrigeration and heating equipment; manufacture of bicycles; manufacture of confectionery products; manufacture of dairy products, not including dairies; manufacture of furniture and fixtures, including cabinets, partitions, and similar small items; manufacture of grain and bakery products; manufacture of cutlery, tableware, hand tools and hardware; manufacture of non-alcoholic beverages; manufacture of office and computing machines; manufacture of plumbing and heating items; manufacture of wearing apparel and accessories; manufacture, assembly, testing and repair of components, devices, equipment and systems of an electrical, electronic, or electro-mechanical nature; outdoor film studio; paper storage and recycling within a building; parcel delivery services; public parks and public playgrounds; religious institutions; rental car agencies including the storage of rental cars; research and development facilities for biomedical, chemical, electronic, mechanical and other scientific purposes; research and manufacture of drugs and pharmaceuticals; schools; warehousing and distribution; water wells and appurtenant facilities. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

Due to the unique nature of large scale recreational development north of Avenue 62, the Mixed Use designation allows for two or more different types of uses contiguous to one another, planned as a unit. For instance, with racetrack development north of Avenue 62, certain

planning areas could develop as residential product types adjacent to racetrack facilities. These planning areas will also allow for duplex units, or structures with two dwelling units placed beside one another sharing a common wall.

A maximum of 159 dwelling units on 46 acres is planned as part of these mixed-use development areas.

### **Commercial-Retail**

A Commercial Retail land use designation, comprising a total of 28.27 acres, is proposed for the project. The purpose of this designation is to provide a range of commercial uses in support of broader employment and residential uses within the Plan. The intent is to: 1) provide limited neighborhood commercial uses to serve residential communities; 2) incorporate commercial uses within walking distance of residential neighborhoods and employment uses; 3) locate highway-oriented commercial uses at the project periphery or along major through roadways, with easy access to and visibility from the arterial highway system; 4) accommodate "big box" commercial uses on larger sites, to serve regional markets; and 5) provide appropriate circulation, parking and loading areas to handle traffic generated by commercial land uses.

In addition to general commercial development, additional uses within the Commercial Retail land use designation may include such uses as public parks and public playgrounds; golf courses; country clubs; animal hospitals with all kennels entirely indoors; health clubs; computer sales and repair stores; parcel delivery services; libraries; religious institutions; community centers; schools; and water wells and appurtenant facilities; and electric vehicle charging stations. Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

### **Public Facilities/Schools**

This land use designation totals 84.30 acres and consists of property owned by the Coachella Valley Unified School District as a site for multiple schools. SPA3SPA4 recognizes that these schools have been constructed and are operational.

In addition to One-Family Dwelling development, additional uses within the Public Facilities/Schools land use designation may include such uses as government offices, courthouses, police stations, fire stations, libraries, museums, public schools, water wells and appurtenant facilities; day care centers; religious institutions; community centers; private schools and communication facilities. Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

### **Open Space**

The purpose of this designation is to provide open space and recreational amenities to serve populations in the project neighborhoods. Approximately 375.94 acres of open space is provided. The intent is to: 1) allow for a variety of passive and active recreational experiences for neighborhood residents and visitors, ranging in scale from community parks to more intimate neighborhood parks; 2) incorporate walking and bicycle trails and linkages between residential neighborhoods and commercial/employment centers, where appropriate; 3) comply with land use restrictions for Jacqueline Cochran Regional Airport safety zones which require the

maintenance of open space clear of obstructions; 4) accommodate stormwater runoff originating both on and off site which must be controlled and conveyed through the property; and 5) provide a variety of landscape settings ranging from highly landscaped to native communities for visual interest as well as buffering between adjoining land uses.

The Specific Plan allows for large-scale recreational uses such as a motor sports race track in the northern portion of the site as a permitted land use. Any such large-scale recreation use, if developed, would be sited around the drainage corridors. The Specific Plan also allows clubhouses and their associated shopping and dining facilities. Additionally, uses such as lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water will be a permitted use in the southern portion of the site. The intent is to: 1) diversify the recreational opportunities available to area residents and visitors; 2) create an attractive amenity around the clubhouses that is enhanced through landscaping and design treatments; 3) develop a central meeting place (clubhouse) that offers uses that are incidental to the primary recreation use; and 4) stimulate job growth and creation of new employment opportunities.

Additionally, light agricultural uses may be allowed when the gross area of a lot is twenty (20) acres or greater. Section 2 – Specific Plan Zoning Ordinance provides an approved list of allowable uses.

## Roads

Major road rights-of-way totaling approximately 176 acres within the project site will be implemented in conjunction with the project.

### 3.1.2 Project-Wide Development Standards

To ensure a logical, orderly, and sensitive development of land uses proposed for the Kohl Ranch, special development criteria, standards, and mitigation measures have been created for each Planning Area. These area-specific standards, discussed in detail in Section 3.2 – Land Use, Planning, and Development Standards by Neighborhood, provide for appropriate transitions to neighboring land uses.

In addition to these specific techniques, project-wide development standards also have been developed as part of the Comprehensive Land Use Plan and are designed to complement the individual conditions within each Planning Area. These general standards are:

- 1) The total Specific Plan shall be developed with a target of 7,162 dwelling units but with a maximum not to exceed 7,171 dwelling units on 1,140.29 acres of residential uses and approximately 48 acres of mixed use, as illustrated on the Specific Land Use Plan. However, the maximum density and the total number of dwelling units within the high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code. General uses permitted will include residential, industrial, commercial, business, open space/golf course, recreational and other lakes, country club, and public facilities.
- 2) Uses and development standards shall be in accordance with the County of Riverside Zoning Ordinance No. 348 for this Specific Plan in Section 2 – Specific Plan Zoning

Ordinance, and shall be defined by Specific Plan objectives, future detailed plot plans and potential conditional use permits as appropriate.

- 3) Standards relating to signage, landscaping, parking and other related design elements shall conform to the County of Riverside Zoning Ordinance No. 348 for this Specific Plan in Section 2, and Coachella Valley Water District Ordinance 1302.1. When appropriate and necessary to meet the goals of this Specific Plan, the standards will exceed the zoning ordinance requirements (see individual Planning Areas, Section 3.2 – Land Use, Planning, and Development Standards by Neighborhood).
- 4) All project lighting shall be in accordance with County of Riverside standards.
- 5) The development of property shall be in accordance with the mandatory requirements of all Riverside County ordinances including Ordinance No. 348 for this Specific Plan in Section 2, Ordinance No. 460, and state laws; and shall conform substantially with the adopted Specific Plan as filed in the office of the Riverside County Planning Department, unless otherwise amended.
- 6) All development on the site will be consistent with this Specific Plan No. 303 and subsequent amendments, as on file with the Riverside County Planning Department, and with all applicable laws of the State of California.
- 7) Prior to issuance of a building permit for construction of any use contemplated by this approval, the applicant shall first obtain clearance from the County of Riverside Planning Department verifying that all pertinent conditions of the Specific Plan approval have been satisfied for the phase of development in question.
- 8) Lots created pursuant to this Specific Plan and any subsequent tentative maps, shall be in conformance with the development standards of the Specific Plan herein applied to the property.
- 9) Agriculture shall continue to be an allowable use in in accordance with Section 2.0 – Specific Plan Zoning Ordinance.
- 10) Utility service to this Specific Plan shall be provided by individual authorized service providers or through the creation of a Public Utilities District (PUD).
- 11) A target maximum number of dwelling units is specified for each residential planning area. The target is based on the applicable density range and may or may not represent the maximum number of units allowed within the density range for any single planning area. Therefore, any given implementing subdivision may increase the number of units beyond the target density up to the maximum allowed for its planning area without a specific plan amendment, provided that the total number of dwelling units within the Kohl Ranch Specific Plan does not exceed 7,171 dwelling units. Further the number of dwelling units contained in an implementing subdivision application may exceed the maximum number of allowable units specified in the Specific Plan for any single planning area by not more than ten percent (10%) without an amendment to this plan, provided that the maximum number of dwelling units in other planning areas are reduced by an equivalent number of units such that the total number of dwelling units within the Kohl Ranch Specific Plan does not exceed 7,171

dwelling units. See Section 3.1.11, Specific Plan Administration, for the approach to tracking units during the build out of the project. Density transfers will be required to receive administrative approval.

- 12) A total of 5 acres of parkland shall be provided for each one-thousand (1,000) in population within the Kohl Ranch Specific Plan boundary or payment of a fee in-lieu thereof, or a combination of both for neighborhood or community park and recreational facilities. Fees shall be paid in accordance with Riverside County Ordinance 460.

### 3.1.3 Circulation Plan

The Kohl Ranch Specific Plan circulation system is designed to provide direct and convenient access to all portions of the project site, and to provide efficient connections to major transportation corridors in the project vicinity such as the new SR-86S Freeway (east of the Whitewater River)<sup>2</sup>. In addition, the proposed circulation system has been coordinated with other planning efforts in the area, such as the planned development at Jacqueline Cochran Regional Airport and the South Valley Implementation Plan. Based on a thorough traffic impact analysis (Appendix J of EIR No. 396), with recommended improvements, the study area intersections at buildout will operate at acceptable levels of service consistent with Riverside County policies. Precise access locations and the phasing of roadway improvements shall be determined at the plot plan or tentative tract map level, subject to approval by the Riverside County Transportation Department.

The project traffic analysis estimates that 88,464 trip-ends per day will be generated by the project at buildout (Appendix J of EIR No. 396). With an internal trip discount of ten percent (the percentage of trips assumed not to leave the Kohl Ranch), it is reduced to 79,618 external trip-ends.

The roadway designations are depicted in **Figure 3.1-2, Circulation Plan**. Roadway cross-sections are depicted in **Figure 3.1-3, Roadway Cross Sections**.

On-site, Avenue 62 is currently identified in the Coachella Valley Association of Governments (CVAG) Transportation Project Prioritization Study (TPPS), between Harrison Street and Polk Street. This is a priority list for transportation projects in the Coachella Valley. Transportation Uniform Mitigation Fees (TUMF), Measure A and other funding sources are administered through CVAG to assist local jurisdictions with roadway improvements. Prioritization on the list is based on the following factors: roadway surface condition, system continuity, level of service, and accident rate. The TPPS is typically updated every five (5) years. Avenue 62 is the only roadway on site that appears on the TPPS list.

#### Public Transit

The SunLine Transit Agency (STA) is the regional public transportation operator in the Coachella Valley and will service the project area. According to the SunLine Transit Facilities Design Manual dated December 2006 and the Bus Route Map for Line 91, effective January 2, 2011, one bus route (Line 91) currently operates along Avenue 66 providing both eastbound and westbound service seven days a week. The nearest bus stop is located at Avenue 66 and Middleton Street located at the southwest corner of the project boundary. EIR 396 includes mitigation measures that require coordination with SunLine Transit, bus stops and bus turn-outs.

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<sup>2</sup> For clarification, please note that the designation SR-86S refers to the new freeway constructed east of the Whitewater River. The "old" Highway 86 is located west of the project site and is referred to in this Specific Plan as Harrison Street, its local street name.

## Circulation Development Standards

- 1) Any application for any subdivision within the specific plan boundary shall comply with the standards of Ordinance 460..
- 2) Each subdivision shall comply with the on-site and off-site street improvement recommendations and mitigation measures outlined in the subsequent traffic studies for each individual project.
- 3) All roadways intersecting four lane facilities or greater shall be a minimum of 74 feet of right-of-way and constructed in accordance with Standard 103, Ordinance 461 from the four-lane facility to the nearest intersection.
- 4) All typical sections shall be per Ordinance 461, or as approved by the Transportation Department.
- 5) All intersection spacing and/or access openings shall be per Standard 114, Ordinance 461, or as approved by the Transportation Department.
- 6) No textured pavement accents will be allowed within County rights-of-way.
- 7) Mid-block cross-walks are not allowed.
- 8) Driveways and access points. No driveways or access points as shown in this specific plan are approved. All access points shall conform to Transportation Department standard access spacing, depending upon the street's classification.
- 9) Drainage. The drainage facilities necessary for this project will generally be outside of the road rights-of-way and maintained by an owners association, county service area, or by the Coachella Valley Water District. Drainage facilities required for road purposes will be maintained by the transportation department or CVWD.
- 10) Commercial. Per the General Plan, "Neighborhood Commercial Uses must be located along Secondary or greater highways, at or near intersections with Secondary Highways."
- 11) Schools/Parks. The Transportation Department's policy regarding streets adjacent to school sites and park sites requires a minimum of 66' right-of-way (Standard 104).
- 12) Any landscaping within public road rights-of-way will require approval by the Transportation Department and assurance of continuing maintenance through the establishment of a landscape maintenance district or similar mechanism as approved by the Transportation Department.
- 13) All trails developed as part of this specific plan shall be designated as shown on **Figure 3.1-10, Public Facility Sites** or as agreed to by Riverside County Regional Parks and Recreation District; and shall be located within or adjacent to the right-of-way pursuant to **Figure 3.4-18, Tyler Street Trail** and **Figure 3.4-19 Avenue 62 Trail**. Class I bikeways shall be developed in accordance with the standards contained within Chapter 1000 of the California Department of Transportation – Highway Design Manual (Fourth Edition).
- 14) All roadway widths will be at the discretion of the Transportation Department, once the ultimate land uses for the individual planning areas are determined.



- 15) All projects, including subdivisions within the specific plan boundary, shall be subject to a Mitigation Monitoring Program, included as part of EIR No. 396 and any updated environmental assessments associated therewith.
- 16) Prior to the issuance of building permits, the landowner shall convey an avigation easement to Jacqueline Cochran Regional Airport, which shall be recorded, or shall provide evidence that such an easement covering the property has already been recorded..

Figure 3.1-2, Circulation Plan

**Figure 3.1-3, Roadway Cross-Sections**

### 3.1.4 Drainage Plan

Drainage flows enter the property from the northwest corner of the site and along the western and northern boundary, drain through the property and outlet along the southeast boundary between Avenues 66 and "D" Street as depicted in **Figure 3.1-4, Conceptual Drainage Plan**. Development of the project will require the collection and conveyance of flood flows through the project in a manner which will ensure the protection of the developed properties from a 100-year flood. In addition, storm flows will be redispersed along the eastern boundary to approximate existing flow conditions, to avoid adversely impacting downstream properties. The proposed flood control system is designed to collect the storm flows as they enter the site in collection basins, transport the flows through the site in graded swales and drainage pipes and discharge the flows over weirs, on the east side of the project.

In order to provide the required level of on-site flood protection, the storm flows will be intercepted on the east side of Tyler Street and on the south side of Avenue 60. The storm flows will then be conveyed through the project area through utilization of drainage pipe. In order to approximate existing historical runoff conditions, the difference between the on-site developed and undeveloped runoff flows will be controlled by use of on-site retention basins. The size and depth of these basins will be determined when a final development plan is prepared. Since each collection basin will be designed to handle predicted flood flows the width of the collection basins will vary. Generally, these collection basins will be trapezoidal in shape with 3:1 side slopes and a top width between 100 and 200 feet. Once the storm flows have passed through the graded swales and drainage pipes they will enter the dispersal basins along the eastern boundary of the project. These basins will vary in top width from 200 to 300 feet and will have a mild gradient toward the south. As peak flows progress in a southerly direction, they will spill over a side weir designed to outlet storm flows toward the east in a manner consistent with existing conditions. Retained water will be pumped in a sheet flow dispersal at rates less than presently occur.

On-site runoff will be intercepted and conveyed through the development by means of a conventional catch basin, swales and storm drain system, in accordance with Coachella Valley Water District (CVWD) standards, so that the increase in on-site runoff resulting from the development will be detained on-site and allowed to percolate into the ground or be captured and reused. The collector storm drain system will be designed to utilize street flow carrying capacity and flows into catch basins and inlets when the quantity exceeds the top of curb.

A high degree of protection from the 100-year flood will be provided to all building pads on the project site as the recommended Flood Control Plan is implemented. Moreover, downstream properties will no longer have to contend with the uncertainty of the existing uncontrolled storm flows, and will have the benefit of controlled flows from the project area.

### Drainage Development Standards

- 1) All drainage facilities will be designed and constructed in accordance with the Riverside County Flood Control and Conservation District (RCFCD) standards and specifications, and the Standard Specifications for Public Works Construction, (GREEN BOOK).
- 2) Drainage facilities will be subject to the review and approval of the Riverside County Transportation Department.
- 3) Design of drainage facilities will be reviewed by CVWD in conjunction with their review of the sewer and water facilities.
- 4) Drainage plans shall be submitted to CVWD for review and approval. This is to ensure that all proposed facilities are compatible with existing CVWD and U.S. Bureau of Reclamation (USBR) facilities.
- 5) The capital cost of all on-site facilities will be the responsibility of the applicant. Such facilities will be dedicated to Riverside County and a homeowners or County Service Area for maintenance and operations.
- 6) All areas within the Specific Plan area will be required to prepare a Storm Water Pollutant Prevention Plan (SWPPP) in accordance with the requirements of the National Pollutant Discharge Elimination System (NPDES) standards. Industrial developments will require an additional SWPPP to operate.
- 7) All projects proposing construction activities including: cleaning, grading, or excavation that results in the disturbance of at least five acres total land area, or activity which is part of a larger common plan of development of five acres or greater shall obtain the appropriate NPDES construction permit and pay the appropriate fees. All development within the specific plan boundaries shall be subject to future requirements adopted by the County to implement the NPDES program. Mitigation measures may include, but not be limited to: on-site retention; covered storage of all outside storage facilities; vegetated swales; monitoring programs; etc.
- 8) The drainage plan for Kohl Ranch shall take into account the existing agricultural drainage facilities in this area. Possible conflicts with these facilities shall be evaluated by the developer's engineer and CVWD.

**Figure 3.1-4, Conceptual Drainage Plan**

### 3.1.5 Conceptual Community Structure Plan

The Community Structure Plan identifies the major project open space and recreation areas which define the project neighborhoods, and the landscape treatments for project roadways and entries.

#### **Open Space and Recreation**

The open space areas are an integral part of the Kohl Ranch Specific Plan. They provide recreational amenities to residents and workers, and are used to screen development edges from potential noise and visual impacts associated with surrounding arterials. The open space system also provides aesthetically pleasing views both from within the project and from surrounding roadways and adjacent properties.

The open space and recreation system for the Kohl Ranch Specific Plan includes neighborhood and community parks, large-scale recreational uses, passive open space and a project-wide trail system. The open space system is organized around, and if necessary, will be adjusted to accommodate any changes in the project drainage network and the Jacqueline Cochran Regional Airport safety zones which limit the land uses and densities which can be located on certain portions of the site. Approximately 376 acres are devoted to the Open Space land use category, including Open Space-Lake. Under a golf course scenario, this would increase by approximately 150 acres.

#### **Large-Scale Recreational Uses**

Allowable land uses could accommodate a golf course that would parallel the drainage system, and serve as a central view focus and buffer between land uses for the majority of residential unit in the southern portion of the project site. The major recreational use in this area could be a golf course, which could alternatively be developed for recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water). The golf course would be equipped with a driving range and clubhouse facility. The golf course is a permitted use allowable under the provisions of the Section 2 - Specific Plan Zoning. If developed, it would be sited to maximize the golf course frontage and views for residential units. While the golf course would be privately owned, it is envisioned that the golf course (or in the alternative, recreational lakes) would serve neighborhood residents.

In the northern portion of the project site, a private motor sports race track, and uses incidental thereto, would be allowed uses within the open space designation in Neighborhood E. The facilities allowed within the open space planning areas in Neighborhood E would include a race track, private garages and uses incidental thereto such as a tuning shop and observation tower.

### Local Parks

Seven local parks are planned to serve residents and/or employees of the project as depicted in **Figure 3.1-5, Community Structure**. If large-scale recreational uses such as a golf course or race track are built, Parks 4, 5, and 6 may be incorporated into the adjacent large-scale recreational facility. The parks are located throughout the site, and are connected by the project-wide trail system (**Figure 3.1-10, Public Facilities Sites**). Under the golf course alternative, an enhanced amenity package is planned which would supplement the recreation opportunities provided by the local parks. **Table 3.1-C, Potential Park Uses** on the following page describes potential park uses for the project, with and without the golf course scenario, **Table 3.1-D, Potential Park Uses (Golf Course Scenario)**, and with or without the racing facility **Table 3.1-E, Potential Park Uses (Racetrack Scenario)**.

- Park 1 – The local park in the northwestern portion of the site located adjacent to Tyler Street, serves the residential uses to the north of Avenue 62. The size of the park would total approximately 10 acres. This park is located to provide easy access to the large area of contiguous natural open space just south of the airport via a project trail along the north side of Tyler Street.
- Park 2 – The community park located to the south of Avenue 62 is approximately 4.0 acres. This park is linked to the other local parks by trails, some of which follow the drainage channels/paseos.
- Park 3 – A 9.1-acre park is located in the center of the site between two residential neighborhoods. This park can be accessed from the north, south, east, and west by the trail system, which also connects the park with public facility sites to the south.
- Parks 4, 5, and 6 – The local parks in the southern portion of the site are intended to be private to serve residents and are connected to each other and the residential neighborhoods they serve via a trail system planned around the lakes. Under the golf course scenario, these parks would be constructed as parts of the golf course.



Figure 3.1-5, Community Structure

**Table 3.1-C, Potential Park Uses<sup>1</sup>**

Park	Description	Size	Potential Park Uses
1	<ul style="list-style-type: none"> <li>▪ Neighborhood park in northwest portion of site in Neighborhood C</li> <li>▪ Serves residential uses to the north of Avenue 62</li> <li>▪ Connected to large area of natural open space via Trail along Tyler Street</li> </ul>	9.97 acres	Ball fields Picnic area Tot lot Play equipment
2	<ul style="list-style-type: none"> <li>▪ Community park south of Avenue 62 in Neighborhood G</li> <li>▪ Within airport safety zone</li> <li>▪ Portion of park within drainage area</li> <li>▪ Linked to other local parks by trails</li> </ul>	4.0 acres	Ball field Picnic area Tot lot Play equipment Tennis courts Recreation center
3	<ul style="list-style-type: none"> <li>▪ Neighborhood park in the center of the site in Neighborhoods G and H</li> <li>▪ Trail system connects park with public facility sites to the north and south</li> </ul>	9.1 acres	Ball fields Picnic area Shuffle board Horse shoes
4 – 6	<ul style="list-style-type: none"> <li>▪ Small Neighborhood parks serving multiple neighborhoods in southern portion of the site (Neighborhoods J and K)</li> <li>▪ Within drainage area</li> <li>▪ Connected to public facilities and other parks through trail system</li> </ul>	6.4 acres	Picnic area Lawn bowling Shuffle board Horse shoes Senior center

1. Potential park uses identified in the table are provided as examples of the types of uses that would be appropriate in each park under the three scenarios. Actual park uses and facilities may differ from those listed.

**Table 3.1-D, Potential Park Uses (Golf Course Scenario)**

Park	Description	Size	Potential Park Uses
1	Same as Table 3.1-C	9.97 acres	Same as Table 3.1-C
2	Same as Table 3.1-C	4.0 acres	Same as Table 3.1-C
3	Same as Table 3.1-C	9.1 acres	Same as Table 3.1-C
4 – 6	Parks could be converted to golf course use	0.0 acres	Not applicable

**Table 3.1-E, Potential Park Uses (Race Track Scenario)**

Park	Description	Size	Potential Park Uses
1	Same as Table 3.1-C	9.97 acres	Same as Table 3.1-C
2	Same as Table 3.1-C	4.0 acres	Same as Table 3.1-C
3	Same as Table 3.1-C	9.1 acres	Same as Table 3.1-C
4 – 6	Same as Table 3.1-C	6.4 acres	Same as Table 3.1-C

### **Trail System**

The proposed development is planned with an off-street (pavement) trail system that parallels streets and drainage ways throughout the site. **Figure 3.1-10, Public Facilities Sites** shows proposed trail locations, however, the final location of the off-street trail system may be adjusted to accommodate final engineering of the drainage system and development plans. A conceptual design for drainage swales and pipeline is shown in **Figure 3.1-4, Conceptual Drainage Plan**. Street-parallel trails will be constructed per the designations identified on **Figure 3.1-10, Public Facilities Sites** and illustrated in the cross sections found in **Figure 3.4-18, Tyler Street Trail** and **Figure 3.4-19, Avenue 62 Trail** found in Section 3.4 – Landscape Design Guidelines. The off-street system is supplemented by sidewalks and bike lanes within the paved roadway. In addition to providing access to the local parks, the trail system also provides a pedestrian linkage to the schools and to key commercial sites. The proposed trail system provides connections through the site to implement its portion of the County-planned trail system for the area.

### **Naturalized Open Space**

The system of drainage channels throughout the project site will offer a scenic amenity to residents of the Kohl Ranch Specific Plan. The channels will be graded and will contain native desert vegetation that will also provide a buffer between land uses. The acreage of this natural open space will vary depending upon whether the golf courses or other recreational uses are built. Planning Area D-1 (110.66 acres) will remain in open space due to airport-related restrictions.

## Landscape Concept

The landscape concept for the Kohl Ranch Specific Plan provides criteria for the treatment of areas within roads and easements, medians, land use transitions, development edges and project entries. The landscape concept has been conceived to organize the development and to establish a unified landscape framework that provides continuity throughout the project area and supports the community themes. Proposed landscape materials are intended to direct and guide the resident or visitor to the development, screen sensitive views, and frame or create focal points and views as the motorist and pedestrian moves through and around the project.

The landscape concept consists of a hierarchy of recommended plantings that correspond to the roadway classifications and project entry statements. More detailed descriptions of these landscape features are provided in Section 3.4 – Landscape Design Guidelines. The landscape concept is based upon the use of natural desert vegetation both for its visual effect and drought tolerant characteristics. The use of water efficient irrigation practices is encouraged, with canal water used for golf courses and public open space areas.

Five basic streetscape planting schemes are proposed:

- Palm Tree Streetscape;
- Formal Canopy Streetscape;
- Informal Canopy Streetscape;
- Windrow Planting; and
- Expressway Planting.

## Project Entries

Major project entries generally are identified by groves of palm trees, which create visual interest through the introduction of a vertical element and a beacon that is visible at a distance across the flat landscape. The palm tree plantings are carried into the site along major project roadways, using a less dense spacing between plantings than the concentrated plantings at project entries. Major project entries are identified at the following locations:

- Avenue 60 at northern project boundary;
- Avenue 62 and Polk Street; and
- "E" Street and Avenue 66.

Minor project entries will be treated with a scaled-down version of the palm tree concept. Entries and intersections will be precisely located as internal roadways are designed.

**Streetscapes**

In order to achieve a cohesive overall circulation system for the Kohl Ranch Specific Plan and provide a strong community structure, a consistent streetscape treatment is recommended for public and private rights-of-way. The general landscape concept combines the use of palms trees with citrus understory, massed in selected locations, interchanged with formal and informal canopy plantings and use of other desert plant material. The streetscapes are broken down into three major categories with special features and plant forms as indicated.

The proposed plantings along major project roadways are based upon the roadway significance and strategic location within the project site. Because of its importance as a connection and northern project entry, Tyler Street, north of Avenue 62, is proposed to employ the palm tree planting concept, with breaks in the plantings where tall trees are prohibited through the airport safety zones. If the race track scenario is developed, Windrow Plantings will be used along its frontage to screen from residential uses to the west.

Formal canopy streetscape plantings are planned along Avenue 62, "B" Street, "C" Street, "E" Street, "F" Street, Tyler Street (south of Avenue 62) and several local roadways that provide internal circulation between neighborhoods. These streets provide definition to the project neighborhoods. Informal canopy streetscape plantings are recommended for local roads that are more interior to the site.

**Buffers and Land Use Transitions**

Windrow plantings are recommended to serve as buffers between land use types, along drainage channels and project edges, and within residential neighborhoods to mitigate against high desert winds. Windrows also can be used to provide structure to project neighborhoods and can be integrated with the project trail system, but must be sited to take into consideration airport safety zones and screening of the race track, if developed.

## Community Structure Development Standards

- 1) All detailed landscaping programs for planning areas and roadways will be prepared by a qualified landscape architect for review by the County.
- 2) Common open space areas within each planning area may be devoted to passive or active uses, and will, to the extent feasible, be coordinated with any open space in adjoining planning areas to create a continuous network. The exact design and layout of facilities will be accomplished in conjunction with detailed future tract layouts.
- 3) Prior to recordation of any final subdivision map, improvement plans for the respective landscaped common areas shall be submitted to the Riverside County Planning Department for approval. The improvement plans shall include, but not be limited to, the following:
  - Final grading plan;
  - Irrigation plans prepared by a landscape architect;
  - A landscaping plan with specifications for mulching and staking methods; locations, type, size and quantity of plantings;
  - Fence treatment plans; and
  - Special treatment/buffer area treatment plans.
- 4) The applicant developer and/or builder shall be responsible for maintenance and upkeep of all common landscaped areas and irrigation systems within its ownership parcels until such time as these operations are the responsibility of other parties.
- 5) At the time of recordation of any final subdivision map which contains a common open space area, the subdivision shall have those common areas transferred to the master property owners association or an appropriate public maintenance agency who will take responsibility for maintenance.
- 6) Irrigation of common open space areas and golf courses will be accomplished with canal water and/or reclaimed water to the extent possible.
- 7) For further landscape development standards, please refer to Section 3.4 – Landscape Design Guidelines.

### 3.1.6 Water and Sewer Plan

The project study area is completely within the service boundary of the Coachella Valley Water District (CVWD) which provides water and sewer service to the project area. The water and sewer plan is based on an analysis of water and sewer infrastructure which evaluates availability of services, calculates water demands and sewer generation quantities, and proposes infrastructure facilities which allow for the advancement of development. The purpose of the water and sewer plan is to provide for the backbone improvements which will be capable of serving the ultimate project, in addition to the individual portions independent of their progression.

#### Existing Water and Sewer Conditions

There are some domestic water distribution facilities within the project boundary. Domestic water provided within the CVWD service boundary comes from ground water which is chlorinated, the Colorado River and the State Water Project. Previous studies conducted in the area indicate that some ground water must be treated for arsenic to meet the State Drinking Water Standards. A Domestic Water, Non-potable Water, Canal Water and Sanitation System Installation and Service Agreement (Document No. 2002-538101) is currently on file at CVWD which provides for mitigation of arsenic when required, through payment of fees for arsenic treatment facilities. CVWD is located at 85-995 Avenue 52 in Coachella, California.

There is an existing tank site southwest of the intersection of Avenue 68 and Harrison Street with a 24-inch waterline that brings water to the project. An 18-inch water line is located in Tyler Street at the intersection of Tyler and Avenue 66 which extends north along the project boundary to Neighborhood I. A 24-inch water line is also located at the intersection of Tyler Street and Avenue 66. This line runs east along the existing school site boundary (Planning Area M-4). An existing arsenic treatment plant is located on the northeast corner of Tyler Street and Avenue 66.

There are some sewer facilities located within the project boundary. An existing sewer line currently services the existing school site (Planning Area M-4) and runs through Neighborhoods J, L, and M to an existing sewage pump station located in Planning Area J-4. An existing 12-inch sewer force main runs north to Avenue 62 and intersects with an existing CVWD force main. It is then pumped to Wastewater Reclamation Plant No. 4.

CVWD operates and maintains its Wastewater Reclamation Plant No. 4 located between Avenue 62 on the north, Avenue 64 on the south, adjacent to the Whitewater River on the east, and approximately 600 feet east of Fillmore Street on the west. This plant has a design capacity of approximately 5.0 MGD, and is estimated to be currently operating at half capacity. The plant currently treats to a secondary treatment level using stabilization ponds for finishing. The ponds are used for flow stabilization by allowing the depth to increase during peak wet water events.

## Proposed Water System and Water Conservation Measures

This project will utilize a “dual water system” consisting of potable and non-potable water systems. The potable system will provide water for domestic purposes and provide water for fire protection. The non-potable system will provide water for irrigation purposes.

The proposed potable water system improvements as depicted in **Figure 3.1-6, Conceptual Potable Water Plan** will operate within a single pressure zone serving ground elevations from 88 to 167 feet below sea level. Hydraulic control for the project should be set using a reservoir high water level of approximately 83 feet above sea level. An existing tank site has been constructed at CVWD’s recommended pad elevation of approximately 60 feet above sea level.

The potable water system is proposed to be fed by wells to be located within or near the project boundary. Four wells are anticipated to pump peak daily demands through an on-site system from 12-inch to 30-inch diameter transmission lines looped for maximum efficiency and fire flow delivery. Internal water lines feeding individual houses will range from 6-inch to 8-inch. Excess water during low and average flows will be conveyed to reservoirs to be located southwesterly as reflected in **Figure 3.1-7, Off-Site Water Improvements**, of the project. An existing 24-inch diameter transmission main will convey flows to the project when operating from gravity flow. Per CVWD’s Master Water Plan, transmission mains are proposed adjacent to, and through the Specific Plan. The Specific Plan water system shall connect to any existing adjacent water facilities that have available capacity.

The potable water system will be designed to deliver gravity flow during high power cost windows. Well pumps will be controlled by level sensing devices and transmitters on the reservoirs. The water system will utilize a certain volume from the tanks, prior to the well pumps cycling, to ensure that adequate circulation of water in the tanks is maintained.

In accordance with the Water Conservation Plan, water conservation measures will be incorporated into the project plan to include water saving devices and systems. Further, the use of surface water will be used for irrigation purposes. Water conservation for the Kohl Ranch project will be achieved through:

- Dual-source water system to reduce groundwater consumption;
- Recycled and/or canal water for irrigation;
- Drip and other efficient irrigation;
- Intelligent irrigation controllers;
- Native and non-native drought-tolerant planting materials;
- Tiered water rates; and
- Use of proposed lake to double as a forebay for the non-potable water treatment.

Groundwater use will be reduced through the initial use of Colorado River water for non-potable water uses as part of a dual-source water system. The design will include a potable water system designed for indoor use only, as well as a separate non-potable water system for outdoor landscaping. The potable water system will be connected to the CVWD domestic water system, which relies upon groundwater. The non-potable system will use Colorado River water delivered by the existing agricultural canal system for landscaping and outdoor use. The majority of the landscaping throughout the project site is designated (in general terms) as “desert landscaping.” And, the project’s proposed high density residential uses reduce the amount of landscaped open space per unit, limits the number of individual pools, and encourages the use



of public facilities.

This non-potable system will be designed to meet recycled water standards in anticipation of the possible future availability of tertiary water when it becomes available from the nearby Water Reclamation Plant 4 (WRP-4). The project will be sewerred and the majority of the demand on the Aquifer from the project will be from indoor use. In the event recycled wastewater from WRP-4 becomes available, project demand on the Aquifer will be further reduced; recycled water can be used to replace Colorado River water or groundwater for agriculture use in the area or on project landscaping. Additional Colorado River water could be freed up for agricultural use or to recharge the Aquifer.

CVWD has three WRP's that produced recycled water for a combined total of 8,289 acre-feet in 2008, which was used to reduce the demand on the groundwater basin. In addition, Desert Water Agency, a partnership with CVWD in the management of the Aquifer, operates a reclamation plant in Palm Springs and produced 3,000 acre-feet of recycled water per year, which also reduces the demand on the groundwater basin. This dual water system is being required of new, larger developments in the Lower Valley, including Kohl Ranch, as a result of SB1557, which finds that the use of potable domestic water for nonpotable uses is wasteful if suitable nonpotable water sources are available.

Additionally, CVWD is working with the cities in its service area to limit the amount of water that can be used for outdoor landscaping. As the result of the adoption of state-wide indoor water conservation measures requiring low flush toilets, shower and faucet flow restrictors, and other devices, the amount of water used inside homes has been significantly reduced. With the large number of new homes constructed, these conservation programs have reduced impacts of new development on the Aquifer.

The non-potable water system as reflected in **Figure 3.1-8, Conceptual Non-Potable Water System**, water source will be diverted canal water that would be delivered into an on-site system of lakes and pond(s). Hydropneumatic booster station(s) will pump water from the lakes and pond(s) throughout the development for irrigation purposes. The on-site water system consists of 8-inch to 42-inch diameter transmission lines looped for maximum efficiency and delivery at appropriate service pressures.

## Proposed Sewer

The Specific Plan's sewer improvements as reflected in **Figure 3.1-9, Sewer Plan**, are based on an analysis of sewer infrastructure that elevates availability of services, calculates sewage generation, and proposes infrastructure facilities which allow for the advancement of the development. The purpose of the sewer plan is to provide for the backbone improvements that will be capable of serving the ultimate project, in addition to the individual portions independent of their progression.

The Specific Plan proposed sewer system conveys flows to the existing sewage pump station and ultimately to CVWD's treatment plant. The Specific Plan sewer system shall connect to any adjacent existing sewer facility that has capacity.

## Water and Sewer Development Standards

- 1) All water and sewer facilities will be designed and constructed in accordance with the CVWD standards and specifications, American Water Works Association (AWWA), American National Standards Institute (ANSI) and the Standard Specifications for Public Works Construction, (GREEN BOOK).
- 2) The capital cost of on-site and off-site facilities necessary to serve the project site will be the responsibility of the applicant. Such facilities will be dedicated to CVWD, after construction, for the District to maintain and operate. Fee credits may be available for transmission, storage, and wells. As discussed above, a Domestic Water, Non-potable Water, Canal Water and Sanitation System Installation, and Service Agreement (Document No. 2002-538101) is currently on file at CVWD which provides for mitigation of arsenic through payment of fees for arsenic treatment facilities.
- 3) In accordance with the Water Conservation Plan (see Appendix F), water conservation measures will be incorporated into the project plan. The following measures will be utilized to reduce the impact on the underground basin and increase efficiency in water usage:
  - (a) A dual water system (potable/non-potable) will be installed to service landscaped areas.
  - (b) Irrigation lines will utilize canal water and/or treated effluent to irrigate landscape areas.
  - (c) Water conservation plumbing fixtures or water saving devices and systems shall be used in all construction.
  - (d) Where possible, the existing tile drains will be maintained to prevent high salt water from migrating to the groundwater basin.
  - (e) The project development shall conform with state, county, and CVWD regulations regarding water conservation and reclamation.
- 4) All development is required to landscape in accordance with **Table 3.4-A, Kohl Ranch Plant Palette** found in Section 3.4, and CVAG Ordinance 1302.1, Landscape and Irrigation System Design Criteria. This will ensure additional water conservation through the use of desert-appropriate landscape materials.

Figure 3.1-6, Conceptual Potable Water Plan

Figure 3.1-7, Off-Site Water Improvements

Figure 3.1-8, Conceptual Non-Potable Water System

Figure 3.1-9, Sewer Plan

### 3.1.7 Public Facilities Sites and Project Phasing

In order to insure timely development of public facilities, a phasing plan has been prepared for parks, schools and other public facilities as depicted in **Figure 3.1-10, Public Facilities Sites**.

#### Project Phasing Plan

Four phasing scenarios have been established for the proposed project as depicted in **Figure 3.1-11, Phasing Plan**. Each scenario represents an area of the site that is likely to develop as a unit. The purpose of a scenario is to establish the infrastructure required to initiate development, assuming that the scenario site is the portion of the total project site to be developed first. However, the scenario numbers do not necessarily indicate the sequence in which the site will be built out. For example, Phasing Scenario 2 and Phasing Scenario 4 could be constructed first. It is anticipated that the project will be developed over a twenty-five year period, in response to market demands, and according to a logical extension of roadways, public utilities and infrastructure. Efforts by local government to stimulate economic development in the project area, through the planned development at Jacqueline Cochran Regional Airport, can be expected to serve as an impetus to development of revenue-producing land uses in the northern portion of the project site. The project phasing scenarios are further described below.

#### Public Facilities Phasing Schedule

Public facilities shall be phased as indicated in **Table 3.1-F, Public Facilities Phasing Schedule**, below.

Table 3.1-F, Public Facilities Phasing Schedule

Planning Area	Public Facility	Size of Site (acres)	Milestones and Requirements (to be determined)
<b>PHASING SCENARIO 1</b>			
K-1/J-2	Lake	44.4	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood K or J.
K-3	Park	3.2	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood K or J.
J-6	Park	2.2	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood K or J.
J-5	Park	1.0	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood K or J.
<b>PHASING SCENARIO 2</b>			
-	-	-	Payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or industrial subdivision map is filed for Neighborhood E.
<b>PHASING SCENARIO 3</b>			
C-5	Park	9.97	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood C.
<b>PHASING SCENARIO 4</b>			
G-3	Park	1.19	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood G.
G-2/G-6	Park	15.09	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood G.
G-12/H-3	Park	9.14	A schedule for development of the park or payment of fees will be submitted to the County Planning Department at the time the first master tentative map and/or residential subdivision map is filed for Neighborhood G or H.

Note: The park acreages assume that the golf course or race track would not be built. Planning Area M-4 is an existing school.



Figure 3.1-10, Public Facilities Sites

Figure 3.1-11, Phasing Plan

**Phasing Scenario 1**

Phasing Scenario 1 encompasses the southern segment of the project area. The infrastructure improvements described in this section assume that Scenario 1 would be the first area of the site to develop. Transportation improvements would involve both major and secondary roadways, including partial width of Polk Street from Avenue 66 to Avenue 64; Avenue 64 from Polk Street to Tyler Street; "F" Street between "E" Street and Tyler Street; and "E" Street between Avenue 64 and Avenue 66. Grading activities for this Scenario would be accomplished by stockpiling export on the Scenario 4 site to the north. Drainage for this scenario would require that off-site flows be picked up along the westerly boundary in master planned facilities, conveyed through the site and then released at a controlled rate along the eastern boundary of Polk. Sewer facilities would gravity flow to the easterly portion of the scenario site to the existing sewage pump station. From that location, an existing sewer force main will convey the flows to the existing waste water lines along Avenue 62. Water facilities to support the development of this scenario begin with the extension by others, of the existing 24-inch diameter waterline in Avenue 66 to the east at the intersection of Avenue 66 and Polk Street. A 24-inch diameter waterline will be constructed by others, in Polk Street between Avenue 66 and Avenue 64. A 12-inch diameter waterline will be constructed in Avenue 64 between Tyler Street and Polk Street in "E" Street and "F" Street. Two wells exist to provide adequate water supply and backup (one on the school site and one on Middleton Street just to the southwest of the project area). Any existing water facilities with capacity shall be used to avoid multiple facilities in the same street.

**Table 3.1-G, Phasing Scenario 1 Details**

Land Use	Planning Areas	Acreage	Units
Medium Density Residential	I-4	7.0	35
	I-5	10.6	51
	I-6	9.6	42
	I-7	6.7	29
	I-8	8.0	40
	I-9	11.5	57
	I-10	12.4	47
	I-11	10.1	37
	J-1	37.52	130
	J-4	43.36	189
	M-1A	2.5	8
	M-1B	18.9	68
	M-1C	16.4	61
	M-5A	4.1	14
	M-5B	15.7	61
	M-6A	3.8	14
	M-6B	13.4	57
M-7A	4.1	20	

Land Use	Planning Areas	Acreege	Units
Medium High Density Residential	M-7B	11	56
	M-7C	5.9	32
	M-7D	15.1	82
	M-7E	15.5	82
High Density Residential	L-1	106.2	1,158
Open Space	I-1	9.6	
	I-2	3.9	
	I-3	0.5	
	J-2	22.0	
	J-3	8.8	
	J-5	1.0	
	J-6	2.2	
	J-7	3.3	
	J-8	1.31	
	K-1	22.4	
	K-2	8.8	
	K-3	3.2	
	L-2	4.1	
	L-3	1.1	
	M-2	1.9	
	M-3	1.6	
M-8	2.9		
M-9	2.7		
<b>Subtotal Scenario 1</b>		<b>490.69</b>	<b>2,370</b>

Note: Planning Area M-4 (84.3 acres) is an existing school site.

**Phasing Scenario 2**

Phasing Scenario 2 is situated in the northeast corner of the project area. The infrastructure improvements described in this section assume that Scenario 2 would be the first area of the site to develop. Transportation improvements would involve expressway, arterial and secondary roadways, including partial width of Polk Street north of Avenue 62 to project boundary; and Avenue 62 between "C" Street and Polk Street. Grading activities for this scenario would be limited to the Scenario 2 site. There would be limited (if any) import/export required off-site. Drainage for Scenario 2 would require that off-site flows be picked up along the western and northern Scenario 2 boundary and in an interim swale along the westerly boundary of Tyler Street and conveyed to the proposed on-site open space channel areas (Planning Areas A-3 and A-7). Off-site flows from these open space areas would be conveyed along a drainage channel running along the west edge of Scenario 2 daylighting south of site through Scenario 4 that returns to a sheet flow condition, with no additional drainage facilities required. Sewer facilities would gravity flow to the southeast corner of the drainage areas to a gravity sewer presently being constructed by CVWD. Water facilities to support the development of this scenario begin with the extension of the existing 24-inch diameter waterline in Avenue 66 east to Polk Street and north in Polk Street to Avenue 60. A proposed 24-inch waterline in Avenue 62 between Tyler Street and Polk Street is also needed. Connecting to these 24-inch lines will be 12-inch lines in Tyler Street, Avenue 60 and internal to the Scenario 2 area to serve the area. Any existing water facilities with capacity shall be used to avoid multiple facilities in the same street.

**Table 3.1-H, Phasing Scenario 2 Details**

Land Use	Planning Areas	Acreage	Units
Mixed Use/Air Park	A-2	24.45	
Mixed Use	A-6	46.18	
	E-2	148.74	
	E-4	12.25	19
	E-5	5.31	120
	E-6	33.99	5
	E-7	1.32	15
Heavy Industrial	E-8	4.94	
	A-4	81.17	
Open Space	A-1	0.63	
	A-3	10.77	
	A-7	9.97	
	D-1	110.66	
	E-1	5.61	
	E-3	69.33	
<b>Subtotal Scenario 2</b>		<b>565.32</b>	<b>159</b>

**Phasing Scenario 3**

Phasing Scenario 3 is located at the northwestern corner of the project area. The infrastructure improvements described in this section assume that Scenario 3 would be the first area of the site to develop. Transportation improvements would involve expressway, arterial and secondary roadways, including "A" Street north of Avenue 62; "B" Street between Avenue 62 and Tyler Street; Tyler Street north of Avenue 62; and partial width of Avenue 62. Grading activities would be limited to the Scenario 3 site. Drainage for Scenario 3 would require that off-site flows be picked up along the western project boundary, conveyed through the site and then released to the adjacent Scenario 2 area. Two small off-site retention facilities may be required east of Scenario 3. The retention facilities would have overflow weirs on the downstream side to return the storm flow to its original sheet flow condition. Sewer facilities would gravity flow to the southeast corner of the site to a gravity sewer in Avenue 62 presently being constructed by CVWD. Water facilities to support the development of this scenario begin with either the extension of the existing 18-inch diameter waterline at Tyler Street and "F" Street to the intersection of Avenue 62 and Tyler Street, or a 24-inch line will be extended westerly from Polk Street in Avenue 62 to its intersection with Tyler Street. From the intersection of Avenue 62 and Tyler Street, a 12-inch diameter waterline will be constructed north on Tyler Street to Avenue 60. From the intersection of Avenue 62 and Tyler Street, a 30-inch waterline will be constructed west to the project boundary. Any existing water facilities with capacity shall be used to avoid multiple facilities in the same street.

**Table 3.1-I, Phasing Scenario 3 Details**

Land Use	Planning Areas	Acreage	Units
Medium Density Residential	B-3	39.01	117
	C-1	40.77	122
Medium High Density Residential	B-1	30	210
	B-2	25.24	177
	B-5	32.70	229
	B-6	26.18	183
	C-2	31.16	218
	C-4	49.83	349
Very High Density Residential	C-8	14.96	105
	C-6	30.85	477
Commercial Retail	A-5	17.61	
Open Space	C-5	9.97	
<b>Subtotal Scenario 3</b>		<b>348.28</b>	<b>2,187</b>

**Phasing Scenario 4**

Phasing Scenario 4 site is located in the central portion of the project area and abuts the southern boundary of the Phasing Scenario 2 and the northern boundary of Phasing Scenario 2. The infrastructure improvements described in this section assume that Scenario 4 would be the first area of the site to develop. Transportation improvements would involve expressway, major and secondary roadways, including partial width of Polk Street between Avenue 62 and Avenue 64; Avenue 64 between Tyler Street and Polk Street; "C" Street from Avenue 64 to Avenue 62; "D" Street from Polk to Avenue 62; Avenue 62 between "C" Street; and Tyler Street between Avenue 64 and the northwest corner of Phase 4. Grading activities for this scenario would be limited to the Scenario 4 site, with the exception of a portion of open space area D-1 in the Scenario 2 site and a portion of open space areas K-1 and J-2 in the Scenario 1 site, which would be used for a borrow/stockpile area or detention/retention areas. Drainage for this scenario would require that off-site flows be picked up in interim swales along the northerly and westerly boundaries, conveyed through the site and then released to the Scenario 1 site immediately to the south. In addition, master planned off-site retention facilities would be required immediately to the south. The retention facilities would have overflow weirs on the downstream side to return the storm flow to its original sheet flow condition. Sewer facilities would either gravity flow to the north to a CVWD gravity line in Avenue 62 or flow through the site to the southeast corner of the scenario to an existing sewer pump station at Polk and Avenue 64. From there, an existing force main will convey the flows to the existing wastewater treatment facility. Water facilities to support the development of this scenario begin with the extension of an existing 24-inch diameter waterline at the intersection of Avenue 66 and Tyler Street to Polk Street and north in Polk Street to Avenue 62. A 24-inch diameter waterline will be constructed in Avenue 62 between Polk Street and "C" Street. A 12-inch diameter waterline will be constructed in Avenue 64 between Tyler Street and Polk Street and within the phase. Two wells will be constructed along the waterline alignment to provide adequate water supply and backup. Any existing water facilities with capacity shall be used to avoid multiple facilities in the same street.

Table 3.1-J, Phasing Scenario 4 Details

Land Use	Planning Areas	Acreage	Units
Medium Density Residential	F-2	71.64	158
	G-5	34.6	69
	G-10	18.57	37
	G-11	26.86	70
	G-13	14.17	53
	H-9	20.03	66
Medium High Density Residential	F-3	79.94	635
	G-7	60.38	432
	H-2	18.65	118
	H-4	22.4	141
	H-5	7.83	45
	H-7	20.99	151
High Density Residential	G-8	20.6	282
	H-8	13.55	189
Commercial Retail	F-4	6.23	
	G-1	4.43	
Open Space	G-2	1.44	
	G-3	1.19	
	G-4	5.6	
	G-6	13.65	
	G-9	6.17	
	G-12	5.35	
	H-1	5.12	
	H-3	3.79	
	H-6	11.43	
	H-10	3.96	
<b>Subtotal Scenario 4</b>		<b>498.57</b>	<b>2,446</b>



## Project Phasing Standards

- 1) Prior to recordation of any final subdivision map, improvement plans for the respective landscaped areas for that stage of development, shall be submitted to the County Planning Department for approval. The improvement plans shall include, but not be limited to the following:
  - Final grading plan.
  - Irrigation plans certified by a landscape architect.
  - A hardscaping plan with location, type and quantity of potential recreational amenities/facilities prepared by a licensed landscape architect.
  - Wall or fencing treatment details consistent with the Specific Plan.

Plans for landscaping, grading and irrigation systems shall be submitted to the Coachella Valley Water District for review. This is to ensure efficient water management.
- 2) Construction of the development permitted hereby, including recordation of final subdivision maps, may be done progressively in stages, provided adequate vehicular access is constructed for all dwelling units in each stage of development and further provided that such phase of development conforms substantially with the Specific Plan Phasing Program.

### 3.1.10 Illustrative Grading Plan

The study area is relatively flat with approximately 45 feet of topographical relief. The existing topography tends to slope from the northwest to the southeast at a nominal rate of between 0.30 percent and 0.40 percent. Earthwork characteristics and grading recommendations as presented in the Geotechnical Feasibility Report were utilized in this study (EIR396 – Appendix B).

The grading concept illustrates site development feasibility and provides a “balanced” earthwork scenario not dependent upon import or export of material. Grading is designed to conform to the drainage conveyance requirements while following existing topographical patterns as reflected in **Figure 3.1-12, Grading Plan**. All development areas are designed with positive drainage towards acceptable drainage conveyances. Prior to the issuance of a grading permit from Riverside County, the developer's engineer shall submit the grading plans to the Coachella Valley Water District for review and approval. This is to ensure that all existing facilities and easements are taken into account prior to construction.

#### Grading Plan Development Standards

- 1) All grading activities shall be in substantial conformance with the overall Conceptual Grading Plan, the California Building Code and Riverside County Ordinance No. 457.
- 2) Prior to any development within any area of the Specific Plan, an overall Conceptual Grading Plan for the portion in process shall be submitted for Planning Department approval. The Grading Plan for each such area shall be used as a guideline for subsequent detailed grading plans for individual stages of development within that area, and shall include preliminary pad and roadway elevations.
- 3) Unless otherwise approved by the County of Riverside, Building and Safety Department, all cut and fill slopes shall be constructed at inclinations of no steeper than two (2) horizontal feet to one (1) vertical foot.
- 4) A grading permit shall be obtained from the County of Riverside, as required by the County Grading Ordinance, prior to grading.
- 5) Soil stabilizers should be used to control dust as required by SCAQMD Rule 403.
- 6) Erosion control practices shall be implemented during grading activities.
- 7) The grading contractor shall be required to obtain an encroachment permit from CVWD prior to entering any right-of-ways which belong to CVWD or USBR.
- 8) All projects proposing construction activities including: clearing, grading, or excavation that results in the disturbance of at least five acres total land area, or activity which is part of a larger common plan of development of five acres or greater shall obtain the appropriate NPDES construction permit and pay the appropriate fees. All development within the specific plan boundaries shall be subject to future requirements adopted by the County to implement the NPDES program.

Figure 3.1-12, Grading Plan

### 3.1.11 Comprehensive Maintenance Plan

Maintenance of open space, recreational facilities and roadways is important in establishing the appearance and quality of the Kohl Ranch Specific Plan area. It is anticipated that maintenance responsibilities for the project will be shared by several entities, as outlined below.

#### **Parks and Recreation**

Local parks will be dedicated to the Coachella Valley Recreation and Parks District (CVRPD), the new special district created to maintain Open Space as described in 8.b, below, or will remain private. The CVRPD, the new district, or Home Owners Association (HOA) will be responsible for park maintenance and recreation programs associated with these facilities.

#### **Open Space**

To maintain project open space (arterial and local streetscapes, trails, drainage courses, and natural open space), a new special district is required, such as a landscaping and lighting district, community services district, or county service area. This decision will be made at a future stage in project development, in concert with County agencies or HOA. The special district would contract with an existing local agency or a private firm for actual services.

#### **Street Lighting**

To operate and maintain local street lighting, a new special district is required, such as a landscaping and lighting district, community services district, or county service area. This decision will be made at a future stage in project development, in concert with County agencies. The special district would contract with an existing local agency or a private firm for actual services.

#### **Drainage Facilities**

CVWD shall not be responsible for the construction, operation or maintenance of stormwater/drainage facilities within the project area. CVWD shall review the hydrology design of the project and evaluate how drainage from the site could affect regional stormwater facilities. The drainage plan shall be subject to mitigation based upon the findings of the CVWD.

Riverside County and a property owners association or County Service Area are assumed to be responsible for maintaining any storm drains, canals or basins within the project area. Riverside County will not be responsible for drainage maintenance unless specifically indicated by the County.

#### **Project Roadways**

All public project roadways and private streets will be designed and constructed to standards acceptable to the County. All public roadways will be entered into the Riverside County System of roads for operation and maintenance as approved by the Board of Supervisors.

### 3.1.12 Airport Zones and Utility Easement Beltway

#### Jacqueline Cochran Regional Airport Land Use Compatibility

The Kohl Ranch site is located immediately south of Jacqueline Cochran Regional Airport, a general aviation transport airport. Based on proximity of the site to the airport, the Specific Plan has been designed to meet the following objectives:

- reflect current use of and future plans for the airport;
- respond to constraints posed by the airport influence area, and ensure land use compatibility with respect to noise, safety and height; and
- maximize opportunities presented by proximity to this resource through careful design of the project circulation system and strategic location of employment-based uses.

In October 1990, the Riverside County Department of Aviation prepared the Thermal Airport Master Plan. In December 2004, the Riverside County Economic Development Agency prepared a new Airport Master Plan for the renamed Jacqueline Cochran Regional Airport. The Master Plan identifies the potential for limited commercial air carrier and air cargo service, and evaluates several alternatives for expansion of the airport to meet current and future demands through the year 2013. Plans for both airside and landside improvements are identified, including: extension of Runway 17-35 across Avenue 60 onto the Kohl Ranch property; retention of Runway 12-30 as the crosswind runway; general aviation terminal through expansion/redevelopment of existing facilities; separate air carrier and air cargo area; additional T-hangars; and heliport. The Master Plan calls out property acquisition of approximately 128 acres south of Avenue 60 for expansion of runway 17-35. The Master Plan also delineates Airport Safety Zones and noise contours related to planned airport operations as reflected in **Table 3.1-K, Airport Safety Zones** and depicted on **Figure 3.1-13, Airport Safety Zones and Utility Easement Beltway**.

In accordance with State requirements, the County Airport Land Use Commission (ALUC) prepared the Comprehensive Land Use Plan (CLUP) for Thermal Airport in August 1992 and updated it in 2005. The CLUP designates an airport influence area and includes land use compatibility guidelines that address airport noise, safety, height restrictions, and general concerns related to aircraft overflight. The "airport influence area" around Jacqueline Cochran Regional Airport is divided into six compatibility zones. Five zones affect the Kohl Ranch Specific Plan, as shown on **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The areas of the Kohl Ranch project amended/modified by [SPA3SPA4](#) are consistent with the land use compatibility guidelines for noise, safety and height contained in the CLUP. The Kohl Ranch project is consistent with the proposed airport expansion and improvement plans described in the Master Plan for the Jacqueline Cochran Regional Airport.

**Figure 3.1-13, Airport Safety Zones and Utility Easement Beltway**

Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway

Table 3.1-K, Airport Safety Zones

Zone		Maximum Density	Maximum Lot Coverage by Structures	Land Use
Symbol	Name			
ISZ	Inner Safety Zone	0 (No structures permitted)	0 (No structures permitted)	No petroleum or explosives storage. No petroleum or natural gas pipelines. No above-ground power lines.
OSZ	Outer Safety Zone	Uses in structures <sup>2</sup> : 25 persons/acre  Uses not in structures: 50 persons/acre	25% of net area	No residential, hotels, places of public assembly, public utility stations/plants, and facilities that process flammable materials, or that could be damaged in an aircraft accident, among others.
ETZ	Emergency Touchdown Zone	0 (No structures permitted)	0 (No structures permitted)	No structures or other objects that would represent obstructions are allowed, such as but not limited to: large trees, heavy fences and walls, tall and steep berms and retaining walls, non-frangible street light and sign standards, billboards.
ERC	Extended Runway Centerline Zone	3 du/net acre. Uses in structures <sup>2</sup> : 100 persons/acre.	50% of gross area or 65% of net area, whichever is greater.	No uses involving as the primary activity, manufacture, storage or distribution of explosives or flammable materials.

Source: Jacqueline Cochran Regional Airport Master Plan, December 14, 2004.

The project supports the development goals for the Jacqueline Cochran Regional Airport by improving circulation in the project vicinity and through the appropriate location of land uses throughout the Kohl Ranch site. Avenue 60 is planned to be closed due to the extension of runway 17-35 across this roadway onto the Kohl Ranch property. Consequently, the Specific Plan proposes the elimination of Avenue 60 where it would intersect with runway 17-35, and proposes Avenue 62 as the primary east-west access through the site.

In addition, land uses planned for the areas closest to the airport property, such as the Mixed Use/AirPark, Mixed Use, Open Space, and Heavy Industrial designations, respond to and support the master planned development intended for the airport. The more intense uses are clustered around the western segment of Avenue 60, as well as along Polk Street, to take advantage of direct access to the airport. Open Space is concentrated in the airport safety zones.

On January 8, 2015, the Riverside County Airport Land Use Commission reviewed Kohl Ranch Specific Plan Amendment No. 3 and found the document to be conditionally consistent with the 2005 Jacqueline Cochran Regional Airport Land Use Compatibility Plan (JCRAUCP). These recommended ALUC conditions will be considered for adoption as conditions of approval by the County.



## Airport Noise Compatibility Guidelines

The CLUP presents noise compatibility guidelines for the Jacqueline Cochran Regional Airport vicinity, as follow:

- Discourage new single-family dwellings and prohibit mobile homes within the 60 CNEL contour. Where homes are permitted within the 60 CNEL contour, the need for sound insulation should be studied and noise easements should be acquired.
- Within the 65 CNEL, new residential construction should not be undertaken. New hotels or motels are permitted if the need for sound insulation is evaluated.
- Institutional uses should be discouraged within the 65-70 CNEL range. If no alternative location is available, the need for sound insulation should be studied.
- Commercial, industrial and recreational uses are considered compatible with noise levels between the 65 and 70 CNEL.

The Kohl Ranch property is impacted by noise from Jacqueline Cochran Regional Airport. To address airport noise in the Specific Plan, primarily open space uses are located within the 65, 70, and 75 CNEL noise contours. A limited amount of airpark/mixed use and industrial uses are within the 65 CNEL contour, consistent with the guidelines. Land uses within the 60 CNEL contour include open space, industrial and commercial retail uses. Some medium density residential uses also are within the 60 CNEL contour. Cluster provisions have been incorporated into Section 2.0 – Specific Plan Zoning so that residential uses can be located outside areas impacted by airport noise. In addition, common walls within medium high, high and very high density units are anticipated to attenuate airport noise within the 60 CNEL contour.

## Airport Vicinity Height Guidelines

The Federal government has developed standards for determining obstructions in navigable airspace. Federal Aviation Regulations Part 77 defines a variety of imaginary surfaces at certain altitudes around airports. The Part 77 surfaces include the primary surface, approach surface, transitional surface, horizontal surface, and conical surface. Collectively, the Part 77 surfaces around an airport define a bowl-shaped area with ramps sloping up from each runway end. The Part 77 standards are not absolute height restrictions, but instead identify elevations at which structures may present a potential safety problem. Penetrations of the Part 77 surface generally are reviewed on a case by case basis. The CLUP uses the Part 77 criteria as the basis for height limitations in the vicinity of Jacqueline Cochran Regional Airport.

Height limitations are not anticipated to pose a development constraint for the Kohl Ranch site. Based on the Part 77 surfaces identified in the CLUP, structures above approximately 85 feet in height in the vicinity of Avenue 62, 150 feet in height in the vicinity of Avenue 64, and 315 feet in height in the vicinity of Avenue 66, could potentially create a problem. Development in these areas is not expected to exceed the relevant height limitations.

## Airport Safety Compatibility Guidelines

The CLUP for Jacqueline Cochran Regional Airport establishes land use compatibility guidelines for land use compatibility zones that affect the project site. These guidelines are summarized below in **Table 3.1-L, Basic Compatibility Criteria**.

In addition to the guidelines for the five zones, the CLUP prohibits particularly hazardous land uses in all designated safety zones. These land uses include uses which would impede the ability of the pilot to see the airfield as identified in Footnote 1 to **Table 3.1-L**.

Open space is concentrated within the most restrictive safety zones, the Emergency Touchdown Zone (ETZ) and the Inner Safety Zone (ISZ), with the exception of a small mixed use land designation in the northeastern most corner of The Kohl Ranch Specific Plan. Land uses proposed in the Specific Plan within the Outer Safety Zone (OSZ) are limited to open space, commercial, and mixed use land uses. The Extended Runway Centerline (ERC) zone, while less restrictive, limits the density of uses within the zone. Consequently, open space and medium density residential uses have been sited within this area. Residential cluster provisions incorporated into Section 2.0 – Specific Plan Zoning, encourage common open space areas to be located within the ERC, with transfer of residential density to areas outside the safety zone.

### **Airport Compatibility Design Standards for Mixed Use Residential**

Mixed Use structures in Planning Areas A-6, and E-2 through E-8 of The Kohl Ranch Specific Plan, allowing for overnight occupancy shall incorporate the following features:

- Signage depicting areas allowing overnight occupancy from areas of non-overnight occupancy.

### **Airport Compatibility Design Guidelines for Lakes**

Recreational lakes within The Kohl Ranch Specific Plan shall be designed to incorporate the following features in order to reduce or eliminate the habitat and foraging opportunities for birds in the water areas:

- Use of linear shaped water areas.
- Formed, vertical banks that make access to land at the water's edge difficult.
- Restrict any vegetation that offers shelter and nesting opportunities.
- Do not include islands in water areas.
- Eliminate shallows and muddy areas.
- Control availability of forage – fish, frogs, algae and insects.
- Provide for human activity in or near the water.

Through the incorporation of these features into the design of the recreational lakes, the lakes will not attract the type or concentration of bird activity that would create a hazard to aircraft in flight.

### **Utility Easement Beltway**

A utility easement beltway traverses the site in an east-west direction, roughly along the Avenue 64 right-of-way. This beltway contains easements for CVWD irrigation pipes, the Avenue 64 Evacuation Channel, and a power line easement for an existing 161 kV power line. This area is proposed to remain in street right of way and open space, and to incorporate an element of the project-wide trail system, connecting with parks and public facilities throughout the site. Residential development in the planning areas immediately to the north of the easement beltway would be clustered outside of the beltway, to maintain the residential densities in these areas and preserve open space areas.

**Table 3.1-L, Basic Compatibility Criteria**

Zone	Locations	Maximum Densities / Intensities				Req'd Open Land <sup>3</sup>	Additional Criteria	
		Residential (d.u./ac) <sup>1</sup>	Other Uses (people/ac) <sup>2</sup>				Prohibited Uses <sup>4</sup>	Other Development Conditions <sup>5</sup>
		Average <sup>6</sup>	Single Acre <sup>7</sup>	with Bonus <sup>8</sup>				
<b>A</b>	Runway Protection Zone and within Building Restriction Line	0	0	0	0	All Remaining	<ul style="list-style-type: none"> <li>&gt; All structures except ones with location set by aeronautical function</li> <li>&gt; Assemblages of people</li> <li>&gt; Objects exceeding FAR Part 77 height limits</li> <li>&gt; Storage of hazardous materials</li> <li>&gt; Hazards to flight<sup>9</sup></li> </ul>	<ul style="list-style-type: none"> <li>&gt; Aviation easement dedication</li> </ul>
<b>B1</b>	Inner Approach/Departure Zone	0.05 (average parcel size ≥20.0 ac.)	25	50	65	30%	<ul style="list-style-type: none"> <li>&gt; Children's schools, day care centers, libraries</li> <li>&gt; Hospitals, nursing homes</li> <li>&gt; Places of worship</li> <li>&gt; Bldgs with &gt;2 aboveground habitable floors</li> <li>&gt; Highly noise-sensitive outdoor nonresidential uses<sup>10</sup></li> <li>&gt; Aboveground bulk storage of hazardous materials<sup>11</sup></li> <li>&gt; Critical community infrastructure facilities<sup>12</sup></li> <li>&gt; Hazards to flight<sup>9</sup></li> </ul>	<ul style="list-style-type: none"> <li>&gt; Locate structures maximum distance from extended runway centerline</li> <li>&gt; Minimum NLR of 25 dB in residences (including mobile homes) and office buildings<sup>13</sup></li> <li>&gt; Airspace review required for objects &gt;35 feet tall<sup>14</sup></li> <li>&gt; Aviation easement dedication</li> </ul>
<b>B2</b>	Adjacent to Runway	0.1 (average parcel size ≥10.0 ac.)	100	200	260	No Req't	Same as Zone B1	<ul style="list-style-type: none"> <li>&gt; Locate structures maximum distance from runway</li> <li>&gt; Minimum NLR of 25 dB in residences (including mobile homes) and office buildings<sup>13</sup></li> <li>&gt; Airspace review required for objects &gt;35 feet tall<sup>14</sup></li> <li>&gt; Aviation easement dedication</li> </ul>
<b>C</b>	Extended Approach/Departure Zone	0.2 (average parcel size ≥5.0 ac.)	75	150	195	20%	<ul style="list-style-type: none"> <li>&gt; Children's schools, day care centers, libraries</li> <li>&gt; Hospitals, nursing homes</li> <li>&gt; Bldgs with &gt;3 aboveground habitable floors</li> <li>&gt; Highly noise-sensitive outdoor nonresidential uses<sup>10</sup></li> <li>&gt; Hazards to flight<sup>9</sup></li> </ul>	<ul style="list-style-type: none"> <li>&gt; Minimum NLR of 20 dB in residences (including mobile homes) and office buildings<sup>13</sup></li> <li>&gt; Airspace review required for objects &gt;70 feet tall<sup>15</sup></li> <li>&gt; Deed notice required</li> </ul>
<b>D</b>	Primary Traffic Patterns and Runway Buffer Area	(1) ≤0.2 (average parcel size ≥5.0 ac.) or <sup>16</sup> (2) ≥5.0 (average parcel size ≤0.2 ac.)	100	300	390	10%	<ul style="list-style-type: none"> <li>&gt; Highly noise-sensitive outdoor nonresidential uses<sup>10</sup></li> <li>&gt; Hazards to flight<sup>9</sup></li> </ul>	<ul style="list-style-type: none"> <li>&gt; Airspace review required for objects &gt;70 feet tall<sup>15</sup></li> <li>&gt; Children's schools, hospitals, nursing homes discouraged<sup>17</sup></li> <li>&gt; Deed notice required</li> </ul>
<b>E</b>	Other Airport Environs	No Limit	No Limit <sup>18</sup>		No Req't	<ul style="list-style-type: none"> <li>&gt; Hazards to flight<sup>9</sup></li> </ul>	<ul style="list-style-type: none"> <li>&gt; Airspace review required for objects &gt;100 feet tall<sup>15</sup></li> <li>&gt; Major spectator-oriented sports stadiums, amphitheaters, concert halls discouraged beneath principal flight tracks<sup>18</sup></li> </ul>	
<b>*</b>	Height Review Overlay	Same as Underlying Compatibility Zone			Not Applicable	Same as Underlying Compatibility Zone	<ul style="list-style-type: none"> <li>&gt; Airspace review required for objects &gt;35 feet tall<sup>14</sup></li> <li>&gt; Aviation easement dedication</li> </ul>	

See Chapter 3 for airport-specific additions or exceptions to these policies

**NOTES:**

- <sup>1</sup> Residential development must not contain more than the indicated number of dwelling units (excluding secondary units) per gross acre. Clustering of units is encouraged. See Policy 4.2.5 for limitations. Gross acreage includes the property at issue plus a share of adjacent roads and any adjacent, permanently dedicated, open lands. Mixed-use development in which residential uses are proposed to be located in conjunction with nonresidential uses in the same or adjoining buildings on the same site shall be treated as nonresidential development. See Policy 3.1.3(d).
- <sup>2</sup> Usage intensity calculations shall include all people (e.g., employees, customers/visitors, etc.) who may be on the property at a single point in time, whether indoors or outside.
- <sup>3</sup> Open land requirements are intended to be applied with respect to an entire zone. This is typically accomplished as part of a community general plan or a specific plan, but may also apply to large (10 acres or more) development projects. See Policy 4.2.4 for definition of open land.
- <sup>4</sup> The uses listed here are ones that are explicitly prohibited regardless of whether they meet the intensity criteria. In addition to these explicitly prohibited uses, other uses will normally not be permitted in the respective compatibility zones because they do not meet the usage intensity criteria.
- <sup>5</sup> As part of certain real estate transactions involving residential property within any compatibility zone (that is, anywhere within an airport influence area), information regarding airport proximity and the existence of aircraft overflights must be disclosed. This requirement is set by state law. See Policy 4.4.2 for details. Easement dedication and deed notice requirements indicated for specific compatibility zones apply only to new development and to reuse if discretionary approval is required.
- <sup>6</sup> The total number of people permitted on a project site at any time, except rare special events, must not exceed the indicated usage intensity times the gross acreage of the site. Rare special events are ones (such as an air show at the airport) for which a facility is not designed and normally not used and for which extra safety precautions can be taken as appropriate.
- <sup>7</sup> Clustering of nonresidential development is permitted. However, no single acre of a project site shall exceed the indicated number of people per acre. See Policy 4.2.5 for details.
- <sup>8</sup> An intensity bonus may be allowed if the building design includes features intended to reduce risks to occupants in the event of an aircraft collision with the building. See Policy 4.2.6 for details.
- <sup>9</sup> Hazards to flight include physical (e.g., tall objects), visual, and electronic forms of interference with the safety of aircraft operations. Land use development that may cause the attraction of birds to increase is also prohibited. See Policy 4.3.7.
- <sup>10</sup> Examples of highly noise-sensitive outdoor nonresidential uses that should be prohibited include amphitheatres and drive-in theaters. Caution should be exercised with respect to uses such as poultry farms and nature preserves.
- <sup>11</sup> Storage of aviation fuel and other aviation-related flammable materials on the airport is exempted from this criterion. Storage of up to 6,000 gallons of nonaviation flammable materials is also exempted. See Policy 4.2.3(c) for details.
- <sup>12</sup> Critical community facilities include power plants, electrical substations, and public communications facilities. See Policy 4.2.3(d) for details.
- <sup>13</sup> NLR = Noise Level Reduction, the outside-to-inside sound level attenuation that the structure provides. See Policy 4.1.6.
- <sup>14</sup> Objects up to 35 feet in height are permitted. However, the Federal Aviation Administration may require marking and lighting of certain objects. See Policy 4.3.6 for details.
- <sup>15</sup> This height criterion is for general guidance. Shorter objects normally will not be airspace obstructions unless situated at a ground elevation well above that of the airport. Taller objects may be acceptable if determined not to be obstructions. See Policies 4.3.3 and 4.3.4.
- <sup>16</sup> Two options are provided for residential densities in *Compatibility Zone D*. Option (1) has a density limit of 0.2 dwelling units per acre (i.e., an average parcel size of at least 5.0 gross acres). Option (2) requires that the density be *greater than* 5.0 dwelling units per acre (i.e., an average parcel size *less than* 0.2 gross acres). The choice between these two options is at the discretion of the local land use jurisdiction. See Table 2B for explanation of rationale. All other criteria for *Zone D* apply to both options.
- <sup>17</sup> Discouraged uses should generally not be permitted unless no feasible alternative is available.
- <sup>18</sup> Although no explicit upper limit on usage intensity is defined for *Zone E*, land uses of the types listed—uses that attract very high concentrations of people in confined areas—are discouraged in locations below or near the principal arrival and departure flight tracks. This limitation notwithstanding, no use shall be prohibited in *Zone E* if its usage intensity is such that it would be permitted in *Zone D*.

Source: Riverside County Airport Land Use Compatibility Plan Policy Document (adopted October 2004)

### 3.1.11 Specific Plan Implementation and Administrative Process

Pursuant to Government Code Section 65451 all specific plans must contain a "program of implementation measures including regulations, programs, public works projects, and financing measures" necessary to implement the specific plan. This section outlines the methods by which development in the specific plan will be processed, including infrastructure financing mechanisms and other necessary programs.

As it relates to the implementation and administrative process, Riverside County Zoning Ordinance 348 shall be adhered to at all times.

#### **Specific Plan Time Frames**

Riverside County requires the adoption of a phasing plan for each Specific Plan (SP). Each plan must be monitored for reasonable progress toward implementation. The phasing program for the Kohl Ranch Specific Plan is described above in Section 3.1.7 – Public Facility Sites and Project Phasing. The project developer will work with the County to adhere to the phasing plan and to ensure the logical and timely completion of the project.

#### **Financing Plan**

The developer shall be responsible for financing construction of the infrastructure improvements required to support the Project, such as perimeter and internal streets, water lines, sewers, and storm drains. All necessary infrastructure improvements shall be developed in conjunction with the roadway improvements. The financing of construction, operation, and maintenance of public improvement and facilities will include funding through a combination of financing mechanisms. However, the developer shall be ultimately responsible for all fair share costs associated with implementing the Project, including but not limited to the costs of providing infrastructure and complying with mitigation measures, conditions of approval, and other requirements of the Project.

Financing may involve a combination of impact fees and exacting, special assessment districts, landscaping and lighting districts, and other mechanisms agreed to by the developer and the County of Riverside as noted below. Developer funded improvements may be subject to a reimbursement agreement or credits against fees pursuant to provisions of a development agreement or conditions of approval. The County and developer will cooperate to ensure that the public facilities are built in accordance with all requirements of the Specific Plan and EIR. A development agreement and conditions of approval may be used to facilitate this process.

#### **Developer Funding**

In many cases, certain onsite facilities are tied directly to individual projects. In these cases, it is reasonable to expect the developer (or property owner) to pay the entire cost of the facility in order to secure development rights. Onsite local streets, utility connections from main trunk lines, and drainage facilities are good examples of facilities that are normally required concurrent with development of an individual parcel funded by the developer.

#### **Special Assessment Districts**

A special assessment district is a type of benefit district that requires a vote by the property owners to encompass a defined and limited geographic area. The County or other agencies may form a special assessment district under one of several different statutory acts to construct

public improvements such as streets, storm drains, sidewalks, streetlights, sewers, parks landscape, and other similar capital facilities. The special assessment district can issue bonds to finance those improvements and levy a special assessment to pay debt service on those bonds.

A special assessment district may fund improvements within the entire Specific Plan area or smaller areas in the Specific Plan where special improvements are constructed that directly benefit only certain property owners. Special assessments districts may only be used to pay for projects that are of specific and direct benefit to the property owner being assessed. The amount of the assessment must directly relate to the amount of benefit received by the property owner.

#### ***Landscaping and Lighting Districts***

Landscaping and lighting districts (LLD) may be used for installation, maintenance, and servicing of landscaping and lighting through annual assessments on benefiting properties. LLDs may also provide for construction and maintenance of appurtenant features, including curbs, gutters, walls, sidewalks or paving, and irrigation or drainage facilities.

#### ***Other Funding Sources***

Other sources may be available to finance improvement projects, such as government grants, private developer coalitions, or various types of bonds not listed above.

## Dwelling Unit Tracking System

Each Residential Planning Area within the Kohl Ranch Specific Plan has an identified density range consistent with the County General Plan, Flexibility Range, and a Target number of dwelling units as identified in **Table 3.1-N, Residential Planning Area Flexibility Range**. The Flexibility Range is consistent with a variation of no more than 10% from the allowable density range. The Flexibility Range was determined based on several factors: (1) ultimately the Planning Area acreage may change as a result of final engineering, (2) the flexibility may be necessary to accommodate airport restrictions which continue to change over time, and (3) based on the fact that there is an overall unit cap within the SP. Flexibility can be given to the density range and target number of dwelling units within each Residential Planning Area, under certain circumstances, as described below. Flexibility can be given to the density range and target number of dwelling units within each residential Planning Area, under certain circumstances, as described below.

Additionally, all Planning Area acreages may be revised when more accurate information is available. Up to a 15 percent variation in Planning Area acreage may be allowed without a Specific Plan Amendment or determination of substantial project conformance with adopted specific plan, but will be required to receive administrative approval. A 16 to 30 percent variation in Planning Area acreage may be allowed with a 'determination of substantial project conformance with adopted specific plan,' and any acreage change in Planning Area acreage of over 30 percent will require a Specific Plan Amendment. A Planning Area acreage change would not result in a Flexibility Range change, without requiring a Specific Plan Amendment. This accommodation should cover reasonable changes in planning area size caused by roadway realignments such as the one that precipitated changes reflected in Specific Plan No. 303 Amendment 2 (SP00303A2).

Any developer proposing residential development within the Kohl Ranch Specific Plan, either through individual or multiple Planning Areas, will be required to report how many dwelling units will be developed within each Planning Area, as shown on **Table 3.1-O, Reporting Mechanism**, and if any modifications to the Land Use Table are necessary. This report will need to be submitted to the County of Riverside Planning Department to ensure the total number of dwelling units will not exceed 7,171. **Table 3.1-P, Master Log**, will be the running tabular record of all proposed and developed dwelling units, for use by the Planning Department.

**Table 3.1-N, Residential Planning Area Flexibility Range**

<b>Planning Area</b>	<b>Land Use Code</b>	<b>Target Dwelling Units</b>	<b>Acres (Label)</b>	<b>Density Range DU/AC</b>	<b>Flexibility Range DU/AC</b>
B-1	Medium High Density Residential (MHDR)	210	30	5.0-8.0	4.5-8.8
B-2	Medium High Density Residential (MHDR)	177	25.24	5.0-8.0	4.5-8.8
B-3	Medium Density Residential (MDR)	117	39.01	2.0-5.0	1.8-5.5
B-5	Medium High Density Residential (MHDR)	229	32.7	5.0-8.0	4.5-8.8
B-6	Medium High Density Residential (MHDR)	183	26.18	5.0-8.0	4.5-8.8
C-1	Medium Density Residential (MDR)	122	40.77	2.0-5.0	1.8-5.5
C-2	Medium High Density Residential (MHDR)	218	31.16	5.0-8.0	4.5-8.8
C-4	Medium High Density Residential (MHDR)	349	49.83	5.0-8.0	4.5-8.8
C-6	Very High Density Residential (VHDR)	477	30.85	14.0-20.0	12.6-22.0
C-8	Medium High Density Residential (MHDR)	105	14.96	5.0-8.0	4.5-8.8
F-2	Medium Density Residential (MDR)	158	71.64	2.0-5.0	1.8-5.5
F-3	Medium High Density Residential (MHDR)	635	79.94	5.0-8.0	4.5-8.8
G-5	Medium Density Residential (MDR)	69	34.6	2.0-5.0	1.8-5.5
G-7	Medium High Density Residential (MHDR)	432	60.38	5.0-8.0	4.5-8.8
G-8	High Density Residential (HDR)	282	20.6	8.0-14.0	7.2-15.4
G-10	Medium Density Residential (MDR)	37	18.57	2.0-5.0	1.8-5.5
G-11	Medium Density Residential (MDR)	70	26.86	2.0-5.0	1.8-5.5
G-13	Medium Density Residential (MDR)	53	14.17	2.0-5.0	1.8-5.5
H-2	Medium High Density Residential (MHDR)	118	18.65	5.0-8.0	4.5-8.8
H-4	Medium High Density Residential (MHDR)	141	22.4	5.0-8.0	4.5-8.8
H-5	Medium High Density Residential (MHDR)	45	7.83	5.0-8.0	4.5-8.8
H-7	Medium High Density Residential (MHDR)	151	20.99	5.0-8.0	4.5-8.8
H-8	High Density Residential (HDR)	189	13.55	8.0-14.0	7.2-15.4
H-9	Medium Density Residential (MDR)	66	20.03	2.0-5.0	1.8-5.5
I-4	Medium Density Residential (MDR)	35	7	2.0-5.0	1.8-5.5
I-5	Medium Density Residential (MDR)	51	10.6	2.0-5.0	1.8-5.5
I-6	Medium Density Residential (MDR)	42	9.6	2.0-5.0	1.8-5.5
I-7	Medium Density Residential (MDR)	29	6.7	2.0-5.0	1.8-5.5
I-8	Medium Density Residential (MDR)	40	8	2.0-5.0	1.8-5.5



I-9	Medium Density Residential (MDR)	57	11.5	2.0-5.0	1.8-5.5
I-10	Medium Density Residential (MDR)	47	12.4	2.0-5.0	1.8-5.5
I-11	Medium Density Residential (MDR)	37	10.1	2.0-5.0	1.8-5.5
J-1	Medium Density Residential (MDR)	130	37.52	2.0-5.0	1.8-5.5
J-4	Medium Density Residential (MDR)	189	43.36	2.0-5.0	1.8-5.5
L-1	High Density Residential (HDR)	1,158	106.2	8.0-14.0	7.2-15.4
M-1A	Medium Density Residential (MDR)	8	2.5	2.0-5.0	1.8-5.5
M-1B	Medium Density Residential (MDR)	68	18.9	2.0-5.0	1.8-5.5
M-1C	Medium Density Residential (MDR)	61	16.4	2.0-5.0	1.8-5.5
M-5A	Medium Density Residential (MDR)	14	4.1	2.0-5.0	1.8-5.5
M-5B	Medium Density Residential (MDR)	61	15.7	2.0-5.0	1.8-5.5
M-6A	Medium Density Residential (MDR)	14	3.8	2.0-5.0	1.8-5.5
M-6B	Medium Density Residential (MDR)	57	13.4	2.0-5.0	1.8-5.5
M-7A	Medium Density Residential (MDR)	20	4.1	2.0-5.0	1.8-5.5
M-7B	Medium High Density Residential (MHDR)	56	11	5.0-8.0	4.5-8.8
M-7C	Medium High Density Residential (MHDR)	32	5.9	5.0-8.0	4.5-8.8
M-7D	Medium High Density Residential (MHDR)	82	15.1	5.0-8.0	4.5-8.8
M-7E	Medium High Density Residential (MHDR)	82	15.5	5.0-8.0	4.5-8.8
<b>TOTAL</b>		<b><u>7,003</u></b>	<b><u>1,140.29</u></b>		



**Table 3.1-P, Master Log**

Planning Case No, TTP, or TPM	Residential PA	Gross Acres	Target Number of SU	Density Ranch DU		Flexibility Range DU		Actual DU Implemented At:			Unit Pool Balance	Unit Balance
								TTM or TTP Approval	Final Map Recordation	Actual Building Permit		

**Development within the Dwelling Unit Range**

The Density Range is established at the time of the adoption of the Specific Plan. Any proposed development within the Density Range is in conformance with the Specific Plan. When completing the Reporting Mechanism, and if the proposed development is within the Density Range, a justification statement is not required, and a Specific Plan Amendment or Specific Plan Substantial Conformance Report is not required, as well.

**Development Below or Above Density Range but Within the Flexibility Range**

The Flexibility Range is defined as the range within 10 percent of the allowable Density Range for each Planning Area. The Flexibility Range gives more flexibility to the Density Range for each Planning Area to account for minor modifications, and is permitted under certain circumstances, without requiring a Specific Plan Amendment, but a Specific Plan Substantial Conformance Report is required.

The Developer, in requesting use of the Flexibility Range, shall state those special circumstances in **Table 3.1-O**, which would justify the use of the Flexibility Range. Examples that would justify use of the Flexibility Range include, but are not limited to: a decrease or increase in Planning Area size at the final engineering level; or the decrease of residential development acreage as a result of the development of parks, trails, or public facilities. The Planning staff shall determine if the request for the Flexibility Range is justified, and in granting approval of the Flexibility Range, staff shall find as follows:

- 1) That the Planning Area for the desired Flexibility Range is adequate in size and shape to accommodate proposed development; and
- 2) That the site for the proposed development contains streets and public facilities properly designed to carry the type and quantity of traffic generated by the subject development; and
- 3) That the proposed development will not impair the integrity and character of the Planning Area in which it is located or otherwise have an adverse effect on adjacent property or the permitted use thereof; and
- 4) That the proposed use is not contrary to this Specific Plan.

**Development Outside of Flexibility Range**

If the proposed development is below or above the existing allowable Density Range and the Flexibility Range, a Specific Plan Amendment will be required.

**The Difference of Dwelling Units Developed Compared to the Target**

Each residential Planning Area has a designated Dwelling Unit Target, which in total comprises 7,161 dwelling units. When a Planning Area is developed, a Residential Development Report, **Table 3.1-O, Reporting Mechanism**, is submitted to the Planning Department to record the number of dwelling units. Any units over or under the Dwelling Unit Target number of dwelling units will be added or taken away from the remaining balance of units to ensure the 7,171 maximum dwelling unit total for the Specific Plan is not exceeded and added or subtracted from the Unit Pool. When the Unit Pool reaches 50 ± units, a redistribution to/from other planning areas shall be completed by the Master Developer and submitted to the County Planning Department.

## 3.2 LAND USE, PLANNING & DEVELOPMENT STANDARDS

Development standards for the Kohl Ranch have been established at four levels: Section 3.1 – Project-Wide Planning Standards, Section 3.3 – Design Guidelines, Section 3.4 – Landscape Design Guidelines, and Land Use, Planning & Development Standards, which are provided in this section.

### Neighborhood Concept

The development proposal for the Kohl Ranch Specific Plan is founded upon the concept of self-sufficient neighborhoods, each defined by strong edges and an internal circulation system, and identified by a similar orientation, intent, and theme. Where appropriate, design guidelines are tailored to reinforce the unique character and theme of the project neighborhoods. Linking all of the neighborhoods is an extensive open space and trail system, relating to the airport safety zones and drainage ways, which provide residents and workers with active and passive recreational opportunities and scenic amenity. This open space network also serves to buffer incompatible uses and neighborhoods from one another as reflected in **Figure 3.2-1, Neighborhoods**. As described earlier in this chapter, neighborhoods in the northern portion of the site generally have a high concentration of employment-based uses, while neighborhoods in the southern portion are predominantly residential.

The Kohl Ranch Specific Plan identifies 95 planning areas on the site grouped within 13 larger areas referred to as neighborhoods. Neighborhoods define logical development areas based on land use and the relationship to planned roadways and infrastructure. The thirteen project neighborhoods were conceived to create cohesive, well-balanced communities, upon completion of the project. The neighborhoods relate generally to the phasing plan for the project, so that infrastructure and amenities can be readily provided as each neighborhood is developed. However, the neighborhood boundaries are slightly different than the boundaries of the project phasing scenarios which are based on the logical provision of infrastructure as well as overall market forces.

Definitions of the project planning areas are based upon logical, separate units of development, adjoining land uses, available infrastructure and physical landscape features.

### Flexibility of the Standards

The Land Use, Planning & Development Standards described here and in Section 2 – Specific Plan Zoning Ordinance, are designed to provide a high degree of flexibility for future development of the Kohl Ranch site. This flexibility is critical, in order to respond to changing market forces during the life of the project, and to accommodate future conditions related to development of Jacqueline Cochran Regional Airport

This flexibility is reflected in provisions for alternate allowable land uses that are specified in the descriptions of neighborhoods and planning areas below. Such alternate land uses include golf courses and related facilities, other large recreational uses such as race track facilities and lakes. The conditions under which alternative land uses are allowed, along with the planned use of buffers and organization of land uses throughout the project will ensure that development of the project can be managed over time and that incompatibilities between adjacent land uses can be avoided.

**Figure 3.2-1, Neighborhoods**

Flexibility is also evident in the plan through the incorporation of provisions which allow the clustering of residential uses, through the transfer of private open space to common open space. As a result, overall residential densities remain the same, and a shared amenity is created. The cluster option is particularly well-suited to land within airport-restricted zones, and to planning areas that are adjacent to drainage facilities which may be expanded under a golf course scenario.

### **Organization of this Section**

Detailed descriptions of the neighborhoods are provided below, followed by neighborhood-wide planning standards which describe airport-related constraints, allowable land uses, neighborhood access, and streetscape, entry and intersection treatments. Following the neighborhood descriptions and planning standards are descriptions and standards for each individual planning area. A brief description, summary of land use and development standards specified in Section 2 – Specific Plan Zoning Ordinance, and applicable planning standards are provided for each planning area. For each planning area, these standards describe such features as alternate allowable land uses and conditions, access, and edge and buffer treatments.

A foldout, plan view graphic is provided for each of the thirteen neighborhoods. Each of these graphics is located at the beginning of the corresponding neighborhood section, and should be kept open during review of the planning area descriptions and standards which precede it. These graphics indicate the location of all planning areas, access points, and all landscape features including streetscapes, intersection details, project entries, edges, and buffers.

Appropriate references in the text are made to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines. The graphics for the Design Guidelines are derived from the Community Structure Plan and are intended to convey the project intent with respect to open space, landscaping, site planning and architecture. These elements will create the framework for development of the site. Although development may conform closely to elements of the illustrative plans provided in this section, it is anticipated that actual development will respond to market conditions.

A Specific Plan Zoning Ordinance was prepared in conjunction with this Specific Plan document. The zoning provisions within that ordinance establish allowable uses and development standards for each planning area. The zoning provisions should be used in conjunction with the planning standards for each planning area.

### 3.2.1 Neighborhood A

Neighborhood A, which consists of seven planning areas, is located in the north-central portion of the project site, just south of Avenue 60 and east of Tyler Street as depicted in **Figure 3.2-2, Neighborhood A**. The neighborhood totals approximately 190 acres including 21.37 acres of open space, 24.45 acres of mixed use/air park, 17.61 acres of commercial retail, 81.17 acres of heavy industrial, and 46.18 acres of mixed use. The neighborhood has a natural outward orientation and response to the regional context. Land uses targeted for Neighborhood A are intended to serve an area larger than the neighborhood itself, providing jobs and services to areas north and west of the site.

The uses provided for within Neighborhood A relate to planned uses at the Jacqueline Cochran Regional Airport located immediately to the north. A mix of commercial, industrial, mixed use, and service uses are intended to support airport development. The open space areas in the neighborhood include elements of the project-wide trail system that are generally coincident with drainage ways and major streets. Primary access to the neighborhood is provided via Avenue 60 and Tyler Street. A major project entry is provided at the intersection of Tyler Street and Avenue 60.

#### NEIGHBORHOOD A PLANNING STANDARDS

- 1) Portions of the commercial retail, mixed use, and industrial land uses within this neighborhood are located within Zone C of the Airport Compatibility Land Use Plan (CLUP). The mixed use/air park area and portions of the commercial-retail, mixed use and industrial uses are located within Zone D as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. Development standards associated with these zones are contained in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- 2) A large-scale recreational use such as a motor sports race track is a permitted use in two of the seven planning areas.
- 3) Tyler Street provides access to Planning Areas A-1 through A-7.
- 4) The northwest corner of Planning Area A-2 at Avenue 60/Tyler Street will be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- 5) The intersections of Tyler Street and Avenue 62 and Tyler Street and “B” Street will be developed in accordance with **Figure 3.4-5, Special Intersection Detail**.
- 6) Avenue 62 will be developed in accordance with **Figure 3.4-6, Expressway Streetscape**.
- 7) Tyler Street, north of Avenue 62 will be developed in accordance with **Figure 3.4-8, Arterial Streetscape 1**.
- 8) Avenue 60 (along north edge of Planning Areas A-2 and A-4) will be developed in accordance with **Figure 3.4-16, Industrial Collector Streetscape**.
- 9) The east side of Tyler Street will be developed in accordance with **Figure 3.4-19, Tyler Street Trail**.
- 10) The north side of Avenue 62 will be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.



Figure 3.2-2, Neighborhood A

**NEIGHBORHOOD A PLANNING AREAS****A-1: OPEN SPACE**

Planning Area A-1 provides for development of 0.63 acres of open space. This area is to be used as a collection point for storm flows entering the project site.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area A-1 is located in Zone D of the CLUP. Development in this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area A-1 is accessed from Tyler Street.
- (3) Landscaping for the Major Project Entry at Avenue 60/Tyler Street will be located in this planning area to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (4) Large-scale recreational use such as a golf course or a motor sports race track is a permitted use in this planning area.
- (5) A Class II Bike Path and Regional Trail connection is located in this planning area along the east side of Tyler Street to be developed in accordance with **Figure 3.4-19, Tyler Street Trail**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

**A-2: MIXED USE/AIR PARK**

Planning Area A-2 provides for 24.45 acres of mixed use/air park development.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area A-2 is located in Zone D of the CLUP. Development in this zone will comply with all applicable restrictions and requirements.
- (2) A large-scale recreational use such as a golf course or a motor sports race track is a permitted use in this planning area.
- (3) Planning Area A-2 is accessed from Tyler Street.
- (4) The edge treatment for the interface between Planning Area A-2's mixed use/air park and the drainage uses in the adjacent planning area, is described in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

**A-3: OPEN SPACE**

Planning Area A-3 provides for the development of 10.77 acres of open space which will be part of the project drainage network.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area A-3 is located in Zones C and D of the CLUP. Development in these zones will comply with all applicable restrictions and requirements.
- (2) A large-scale recreational use such as a motor sports race track is a permitted use in this planning area.
- (3) Planning Area A-3 is accessed from Tyler Street.
- (4) Landscaping for the Major Project Entry at Avenue 60/Tyler Street will be located in this planning area to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (5) The edge treatment for the interface between the drainage uses in this planning area and the heavy industrial and mixed use/air park land uses in the adjacent planning areas are described in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (6) A Class II Bike Path and Regional Trail connection is located in this planning area along the east side of Tyler Street to be developed in accordance with **Figure 3.4-18, Tyler Street Trail**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Project Wide Development Standards.

**A-4: HEAVY INDUSTRIAL**

Planning Area A-4 provides for 81.17 acres of heavy industrial development.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area A-4 is located in Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development in this zone will comply with all applicable restrictions and requirements.
- (2) A large-scale recreational use such as a golf course or a motor sports race track is a permitted use in this planning area.
- (3) Planning Area A-4 is accessed from Tyler Street.
- (4) The edge treatment for the interface between Planning Area A-4's heavy industrial use and the drainage uses in the adjacent planning area is described in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

**A-5: COMMERCIAL RETAIL**

Planning Area A-5 provides for 17.61 acres of commercial development.

***Land Use and Development Standards***

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

***Planning Standards***

- (1) Planning Area A-5 is located within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development in this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area A-5 is accessed from Tyler Street and Avenue 62.
- (3) The edge treatment for the interface between the commercial use development in this planning area and the drainage uses in the adjacent planning area is described in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (4) A Class II Bike Path and Regional Trail connection is located in this planning area along the east side of Tyler Street to be developed in accordance with **Figure 3.4-19, Tyler Street Trail**. A Class I Bike Path is located in this planning area along the north side of Avenue 62 to be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

**A-6: MIXED USE**

Planning Area A-6 provides for 46.18 acres of mixed-use development.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area A-6 is located within Zones C and D of the CLUP for the Jacqueline Cochran Regional Airport. Development in these zones will comply with all applicable restrictions and requirements.
- (2) A large-scale recreational use such as a motor sports race track is a permitted use in this planning area.
- (3) Planning Area A-6 is accessed from Tyler Street.
- (4) The edge treatment for the interface between the drainage uses and the heavy industrial land uses in the adjacent planning area is described in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (5) A Class II Bike Path and Regional Trail connection is located in this planning area along the east side of Tyler Street to be developed in accordance with **Figure 3.4-18, Tyler Street Trail**.
- (6) An observation tower built within Planning Area A-6 and built as part of a large scale recreational use shall not exceed 70 feet.
- (7) All lighting fixtures, including spot lights, electrical reflectors and other means of illumination for signs, structures, landscaping, parking, loading, unloading and similar areas, shall be focused, directed, and arranged to prevent glare or direct illumination on streets or adjoining property.
- (8) Sports lighting, consisting of exterior nighttime lighting for ballfields, racetracks, and other sporting activities, shall not be permitted.
- (9) Please refer to Section 3.3 – Design Guidelines for Mixed Use Development and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (10) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

**A-7: OPEN SPACE**

Planning Area A-7 provides for 9.97 acres of open space which will be part of the project drainage network.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area A-7 is located within Zone C of the CLUP for the airport. Development in this zone will comply with all applicable restrictions and requirements.
- (2) A large-scale recreational use such as a golf course or a motor sports race track is a permitted use in this planning area.
- (3) Planning Area A-7 is accessed from Tyler Street and Avenue 62.
- (4) The edge treatment for the interface between the drainage in this planning area and the heavy industrial and commercial land uses in the adjacent planning areas is described in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (5) A Class I Bike Path is located in this planning area along the north side of Avenue 62 to be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

**A-8:** *Planning Area A-8 has been combined with adjacent planning area E-4 to provide a new Planning Area E-4 described below in Section 3.2.8.*

### 3.2.2 Neighborhood B

Neighborhood B contains five planning areas and is comprised of 153.13 acres as reflected in **Figure 3.2-3, Neighborhood B**. It is located in the northwestern portion of the project site, just south of "B" Street and Tyler Street. Proposed land uses are focused along Tyler Street, and include Medium Density Residential (39.01 acres) and Medium High Density Residential (114.12 acres). A target of 916 residential units is planned for Neighborhood B. These units are intended to provide housing for employees of the business, commercial and industrial areas in the northern portion of the project site. Primary access to the neighborhood is provided via Tyler Street and "B" Street. A major project entry is provided at the Avenue 60/Tyler Street intersection.

#### NEIGHBORHOOD B PLANNING STANDARDS

- (1) The entire neighborhood is located within Zone D of the CLUP for the airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to this zone are described in Section 3.1 – Project Wide Planning Standards of this Specific Plan as depicted in **Table 3.1-G, Basic Compatibility Criteria**.
- (2) Avenue 60 provides access to Planning Areas B-1.
- (3) Tyler Street provides access to Planning Areas B-2, B-5 and B-6.
- (4) "B" Street provides access to Planning Areas B-2, B-3, B-5 and B-6.
- (5) A Major Project Entry is located in the northwest corner of the neighborhood at Avenue 60/Tyler Street to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (6) A Special Intersection Detail is provided for the intersection of "B" Street and Tyler Street to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (7) Tyler Street will be developed in accordance with **Figure 3.4-8, Arterial Streetscape 1**.
- (8) "B" Street will be developed in accordance with **Figure 3.4-17, Collector Streetscape 1**.

**Figure 3.2-3, Neighborhood B**



**NEIGHBORHOOD B PLANNING AREAS****B-1: MEDIUM HIGH DENSITY RESIDENTIAL**

Planning Area B-1 provides for the development of 30.00 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 210 dwelling units is planned within a density range of 5.0–8.0 du/acre.

***Land Use and Development Standards***

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

***Planning Standards***

- (1) Planning Area B-1 is accessed from Tyler Street.
- (2) Landscaping for the Major Project Entry at Avenue 60/Tyler Street will be located in this planning area to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (3) This planning area will be developed with the project edge treatment as depicted in **Figure 3.4-29, Out Parcel/Adjacent Property Buffer**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project-Wide Planning Standards.

**B-2: MEDIUM HIGH DENSITY RESIDENTIAL**

Planning Area B-2 provides for the development of 25.24 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning). A target of 177 dwelling units is planned within a density range 5.0–8.0 du/acre.

***Land Use and Development Standards***

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

***Planning Standards***

- (1) Planning Area B-2 is accessed from Tyler Street and “B” Street.
- (2) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (3) Please refer to Section 3.1 – Project-Wide Planning Standards.

**B-3: MEDIUM DENSITY RESIDENTIAL**

Planning Area B-3 provides for the development of 39.01 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 117 dwelling units is planned within a density range of 2.0–5.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (see Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area B-3 is accessed from “B” Street.
- (2) This planning area will be developed with the project edge treatment as depicted in **Figure 3.4-29, Out Parcel/Adjacent Property Buffer.**
- (3) Please refer to Section 33.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (4) Please refer to Section 3.1 – Project-Wide Planning Standards.

**B-4:** *Planning Area number has intentionally been skipped in the numbering sequence.*

**B-5: MEDIUM HIGH DENSITY RESIDENTIAL**

Planning Area B-5 provides for the development of 32.7 acres of medium density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development. A target of 229 dwelling units is planned within a density range of 5.0–8.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (see Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area B-5 is accessed from “B” Street and Tyler Street.
- (2) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (3) Please refer to Section 3.1 – Project-Wide Planning Standards.

**B-6: MEDIUM HIGH DENSITY RESIDENTIAL**

Planning Area B-6 provides for the development of 26.18 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 183 dwelling units is planned within a density range of 5.0–8.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area B-6 is accessed from “B” Street and Tyler Street.
- (2) This planning area will be developed with the project edge buffer conditions as indicated in in **Figure 3.4-29, Out Parcel/Adjacent Property Buffer** .
- (3) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (4) Please refer to Section 3.1 – Project-Wide Planning Standards.

**PLANNING AREA B-7:** *Planning Area number has intentionally been skipped in the numbering sequence.*

### 3.2.3 Neighborhood C

Neighborhood C is located just north of Avenue 62 in the northwestern portion of the project site as reflected in **Figure 3.2-4, Neighborhood C**. This 177.54-acre neighborhood contains the following land uses within its six planning areas: Medium Density Residential (40.77 acres), Medium High Density Residential (95.95 acres), Very High Density Residential (30.85 acres) and Open Space (9.97 acres). A target of 1,271 dwelling units is planned for Neighborhood C. The predominantly residential uses are intended to house employees working in the employment-based neighborhoods located nearby. The maximum density and the total number of dwelling units within the very high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code. The proximity of these residential areas to job opportunities is designed to reduce the number and length of home-to-work trips that would otherwise be generated by these land uses. The higher density residential uses are located in closer proximity to commercial uses, while the lower density residential uses are located at the project periphery. If developed with a golf course, a permitted use under provisions of Section 2 - Specific Plan Zoning Ordinance, residents would be provided with a high-quality recreational and scenic amenity. A number of the neighborhood planning areas would be afforded golf course frontage. In the absence of the golf course, drainage ways would function to provide more natural open space, providing structure and definition to the neighborhood. In addition, a local park would be developed to serve neighborhood residents and act as a buffer between adjacent residential uses and land uses located across Tyler Street that could allow for development of a kart track or driver training facility. Primary access to the neighborhood is from "A" Street, "B" Street, Tyler Street and Avenue 62.

#### NEIGHBORHOOD C PLANNING STANDARDS

- (1) Neighborhood C is located within Zone D of the CLUP for the Jacqueline Cochran Regional Airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. Development standards associated with this zone are contained in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) A golf course use is a permitted use in all planning areas as defined in Section 2 – Specific Plan Zoning Ordinance.
- (3) Avenue 62 provides access to Planning Areas C-2 and C-6.
- (4) "A" Street provides access to Planning Areas C-1 and C-2.
- (5) "B" Street provides access to Planning Areas C-1, C-2, C-4, C-6 and C-8.
- (6) Tyler Street provides access to Planning Areas C-5 and C-6.
- (7) A Minor Entry is located at "B" Street and Avenue 62 to be developed in accordance with **Figure 3.4-4, Minor Entry**.
- (8) "B" Street will be developed in accordance with **Figures 3.4-17, Collector Streetscape 1**.
- (9) "A" Street will be developed in accordance with **Figures 3.4-18, Collector Streetscape 2**.
- (10) Tyler Street north of Avenue 62 will be developed in accordance with **Figures 3.4-8, Aerial Streetscape 1**.
- (11) Avenue 62 will be developed in accordance with **Figures 3.4-6, Expressway Streetscape**.
- (12) A local park will be provided in Planning Area C-5, to serve neighborhood residents and provide a buffer between adjacent residential uses and land uses located across Tyler Street that could allow for development of a kart track.
- (13) The north side of Avenue 62 will be developed with **Figure 3.4-20, Avenue 62 Trail** as reflected in **Figure 3.1-10, Public Facilities**.

**Figure 3.2-4, Neighborhood C**

**NEIGHBORHOOD C PLANNING AREAS****C-1: MEDIUM DENSITY RESIDENTIAL**

Planning Area C-1 provides for the development of 40.77 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 122 dwelling units is planned within a density range of 2.0–5.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area C-1 is located within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course is a permitted use in this planning area.
- (3) Planning Area C-1 is accessed from “A” Street and “B” Street.
- (4) The Project Edge Treatment for this planning area is illustrated in **Figure 3.4-29, Out Parcel/Adjacent Property Buffer**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

**C-2: MEDIUM HIGH DENSITY RESIDENTIAL**

Planning Area C-2 provides for the development of 31.16 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development (Section 2 – Specific Plan Zoning). A target of 218 dwelling units is planned within a density range of 5.0–8.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area C-2 is located within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course is a permitted use in this planning area.
- (3) Planning Area C-2 is accessed from “A” Street, “B” Street and Avenue 62.
- (4) The Project Edge Treatment for this planning area is illustrated in **Figure 3.4-29, out Parcel/Adjacent Property Buffer**.
- (5) A Minor Entry is planned for the intersection of “B” Street and Avenue 62 as shown in **Figure 3.4-4, Minor Project Entry**.
- (6) A Class I Bike Path is located in this planning area along the north side of Avenue 62 as illustrated in **Figure 3.4-20, Avenue 62 Trail**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

**C-3:** *Planning Area number has intentionally been skipped in the numbering sequence.*

#### **C-4: MEDIUM HIGH DENSITY RESIDENTIAL**

Planning Area C-4 provides for the development of 49.83 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet under conventional lots and 4,000 square feet for cluster development as defined in Section 2 – Specific Plan Zoning). A target of 349 dwelling units is planned within a density range of 5.0–8.0 du/acre).

##### **Land Use and Development Standards**

Please refer to Ordinance No. 348 (see Section 2 –Specific Plan Zoning).

##### **Planning Standards**

- (1) Planning Area C-4 is located within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course is a permitted use in this planning area.
- (3) Planning Area C-4 is accessed from "B" Street.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Development Plans and Standards site-wide standards.

#### **C-5: OPEN SPACE**

Planning Area C-5 provides for the development of 9.97 acres of open space that will be developed as a local park and provide a buffer between adjacent residential uses and land uses located across Tyler Street that could allow for mixed-use development.

##### **Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

##### **Planning Standards**

- (1) Planning Area C-5 is located within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course is a permitted use in this planning area.
- (3) Planning Area C-5 is accessed from Tyler Street.
- (4) This planning area will be developed in accordance with **Figure 3.4-29, Out Parcel/Adjacent Property Buffer.**
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

**C-6: VERY HIGH DENSITY RESIDENTIAL**

Planning Area C-6 provides for the development of 30.85 acres of very high density residential land uses with minimum lot sizes of 4,000 square feet for conventional lots and 2,000 square feet under cluster development. A target of 477 dwelling units is planned within a density range of 14.0–20.0 du/acre. The maximum density and the total number of dwelling units within the very high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area C-6 is located within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area C-6 is accessed from “B” Street, Tyler Street and Avenue 62.
- (3) A Minor Entry is planned for the intersection of “B” Street and Avenue 62 to be developed in accordance with **Figure 3.4-4, Minor Entry**.
- (4) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) A Class I Bike Path is located in this planning area along the north side of Avenue 62 to be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.
- (6) The Commercial Buffer treatment for the interface between the residential uses in this planning area and the commercial uses located across the street is illustrated in **Figure 3.4-31, Commercial Buffer Streetscape**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Project Wide Development Standards.

**C-7:** *Planning Area number has intentionally been skipped in the numbering sequence.*



**C-8: MEDIUM HIGH DENSITY RESIDENTIAL**

Planning Area C-8 provides for the development of 14.96 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development. A target of 105 dwelling units is planned within a density range of 5.0–8.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area C-8 is located within Zone D of the CLUP for the Desert Resorts Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area C-8 is accessed from "B" Street through interior loop roads.
- (3) This planning area will be developed with the Buffer Condition along the adjacent property to the east which is depicted in **Figure 3.4-29, Out Parcel/Adjacent Property Buffer**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

### 3.2.4 Neighborhood D

Neighborhood D is located in the northern portion of the project site, between Avenue 60 and Avenue 62 as depicted in **Figure 3.2-5, Neighborhood D**. This neighborhood is intended to function as a buffer between the business, commercial, recreational uses and the airport runway to the north. The neighborhood is 110.66 acres in size and consists of open space uses. All of the open space is within airport influenced areas and is designed to handle storm drainage. Primary access to this neighborhood is provided via Avenue 60 from the west or the east, but Avenue 60 will not traverse the northern edge of the Neighborhood due to possible airport expansion.

#### NEIGHBORHOOD D PLANNING STANDARDS

- (1) The open space neighborhood is constrained by the OSZ, Inner Safety Zone (ISZ) and ETZ airport zones as reflected in **Figure 3.1-13, Airport Zones**. Additionally, Neighborhood D is located within Zone A of the CLUP for the Jacqueline Cochran Regional Airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to these zones are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) Avenue 60 provides access to Planning Area D-1.

Figure 3.2-5, Neighborhood D

**NEIGHBORHOOD D PLANNING AREAS****D-1: OPEN SPACE**

Planning Area D-1 provides for 110.66 acres of open space. A portion of this area will be used to handle storm flows. Much of the planning area is described in the Jacqueline Cochran Regional Airport Master Plan as proposed for acquisition for the runway clear zone. This planning area shall remain in open space uses.

***Land Use and Development Standards***

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

***Planning Standards***

- (1) Planning Area D-1 is located within the Jacqueline Cochran Regional Airport ETZ and ISZ airport safety zones as reflected on **Figure 3.1-13 Airport Zones** and **Table 3.1-F, Airport Safety Zones**. Development within these zones will comply with all applicable restrictions and requirements. Additionally, Neighborhood D is located within Zone A of the CLUP for the Jacqueline Cochran Regional Airport as reflected in reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. Development standards associated with this zone are contained in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) Planning Area D-1 is accessed from Avenue 60.
- (3) The Buffer Condition between Planning Area D-1 and adjacent industrial uses will be as illustrated in **Figure 3.4-6, Expressway Streetscape**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

### 3.2.5 Neighborhood E

Neighborhood E is situated in the northeastern corner of the project site and is bounded by Jacqueline Cochran Regional Airport and Avenue 60 on the north, Polk Street on the east, and Avenue 62 on the south as depicted in **Figure 3.2-6, Neighborhood E**. This neighborhood has a strong regional orientation as a result of its excellent access to the regional circulation system and to Jacqueline Cochran Regional Airport. The neighborhood also looks eastward by virtue of its potential connection to the proposed interchange at Avenue 62 and the new Highway 86, just a few miles to the east. Primary access is provided by Polk Street, Avenue 60 and Avenue 62. The land uses proposed in this 281.49-acre neighborhood include mixed-use (206.55 acres), and open space (74.94 acres).

A racetrack has been approved for this neighborhood with its own separate set of conditions of approval and design manual for features specific to its development. The open space in the northeastern portion of the neighborhood responds to airport-related constraints, and will become part of the larger recreational land use.

Land designated for mixed use development abutting the airport property offers the potential for airport uses to extend southward into the site. This area also has potential for rail service to be provided from the north via a rail spur from the Southern Pacific main line. In addition to the jobs that could be created by heavy industry, Neighborhood E allows for a large-scale recreational use such as a motor sports race track which will also create jobs. Agricultural operations to the east of Polk Street offer the potential for development of compatible agriculture-related uses on the property. A Major Entry is located at the intersection of Polk Street and Avenue 62.

#### NEIGHBORHOOD E PLANNING STANDARDS

- (1) Portions of Neighborhood E are constrained by the OSZ and ETZ airport safety zones as reflected in **Figure 3.1-13, Airport Zones**. Additionally, the neighborhood is located within Zones B1, C, and D of the CLUP as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to these zones are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan. As Neighborhood E allows for mixed-use residential uses in designated planning areas, **Figure 3.2-6A, Neighborhood E Mixed Use Residential Lots**, identifies which lots within those planning areas allow for such use. Where a residential lot overlaps two airport land use compatibility zones, the least restrictive zone policy applies. For example, Lot 55 on Figure 3.2-6A lies within both Zone C and D. Because Zone D is the less restrictive zone, Zone D ALUC policies apply.
- (2) ~~Avenue 62-Tyler Street~~ provides access to Planning Areas E-2, E-3, E-4, E-5, E-6, E-7, and E-8 ~~until such time as access is developed as part of through~~ Planning Area A-6.
- (3) Avenue 60 provides access to Planning Area E-1 and E-2.
- (4) Polk Street provides access to Planning Area E-2, E-5, E-6, E-7, and E-8.
- (5) A Major Project Entry is located at ~~Avenue 62-Tyler Street~~ and Polk Street to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- ~~(6) A Special Intersection Detail is located at "C" Street and Avenue 62 to be developed in accordance with Figures 3.4-5, Intersection Detail.~~
- ~~(7)(6)~~ Polk Street will be developed in accordance with **Figure 3.4-9, Arterial Streetscape 2 – Polk Street Streetscape 2A**.
- ~~(8)(7)~~ Avenue 62 will be developed in accordance with **Figure 3.4-6, Expressway Streetscape**, with the exception of the ETZ in which no trees are to be planted.
- ~~(9)(8)~~ A large-scale recreational use such as a motor sports race track is a permitted land use in seven of the eight planning areas as defined in Section 2 – Specific Plan Zoning Ordinance.

~~(10)(9)~~ The north side of Avenue 62 will be developed as depicted in **Figure 3.1-10, Public Facilities** in accordance with **Figure 3.4-20, Avenue 62 Trail**.

~~(11)(10)~~ The west side of Polk Street will be developed with a Class II Bike Path as described **Figure 3.1-10, Public Facilities**.

Figure 3.2-6, Neighborhood E

Figure 3.2-6A, Neighborhood E Mixed Use Residential Lots



**NEIGHBORHOOD E PLANNING AREAS****E-1: OPEN SPACE**

Planning Area E-1 provides for the development of 5.61 acres of open.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area E-1 is located within Zone B1 of the Jacqueline Cochran Regional Airport CLUP and the ETZ and OSZ airport safety zones. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Large-scale recreational uses such as a motor sports race track are permitted uses in this planning area.
- (3) Planning Area E-1 is accessed from Avenue 60.
- (4) Large-scale recreational uses such as a golf course and a motor sports race track are permitted uses in this planning area.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

**E-2: MIXED USE**

Planning Area E-2 provides for the development of 148.74 acres of mixed-use ~~nonresidential~~ development.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area E-2 is located within Zone C of the Jacqueline Cochran Regional Airport CLUP. This planning area will comply with all applicable restrictions and requirements of that zone.
- (2) Access to Planning Area E-2 is from Tyler and Polk Streets.
- (3) The edge treatment for the interface between the planned mixed use and adjacent open space is illustrated in **Figure 3.4-28, Airpark Drainage Buffer**. If the race track is developed, the edge treatment will be as described in **Figure 3.4-24, Golf Course/Racetrack Edge Condition**.
- (4) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) Large-scale recreational uses such as a motor sports race track are permitted uses in this planning area ~~including racetrack recreational units as defined below. Signage shall be posted that overnight occupancy is prohibited in non-residential structures.~~
- (6) Standards for Racetrack Recreational Unit development:
  - a. A maximum of 110 racetrack recreational units with minimum lot sizes of 7,000 square feet, as defined in Section 2-Specific Plan Zoning, may be provided in Planning Area E-2.
  - b. No buffer is required for uses contiguous to this Planning Area.
  - c. Standard Setbacks
    - i. No minimum setback is required from any private street.
    - ii. Front Yard: 30-foot minimum
    - iii. Rear Yard: No minimum

- iv. Side Yard: 5-feet.  
 d. Structures must provide vehicle storage area on the ground floor of structure and may include the following:

Garage Level (1<sup>st</sup> Floor):

- Cars
- Lifts
- Work areas
- Storage
- Mechanical
- Main Entry
- Half baths
- Bath with locker rooms
- Washer/dryer area
- Elevator
- Stairs
- Media Room
- Kitchenette area

Second Level (and/or mezzanine):

- Elevator
- Stairs
- Storage
- Mechanical
- Bedrooms and closets
- Bath area
- Laundry
- Family Room/Game Room
- Kitchen
- Outside Patios
- Outside Planters

Third Level (Main Living Level):

- Bedrooms and Closets
- Bath
- Elevator
- Stairs
- Laundry
- Kitchen
- Dining
- Great Room
- Outside Patios
- Mechanical
- Storage
- Game Room
- BBQ area
- Outside pools and hot tub
- Planters

- e. Access to structures shall be from common access areas or corridors

~~(5)~~ \_\_\_\_\_

~~(6)~~~~(7)~~ No minimum setback or buffer is required for Planning Area E-2 from adjacent residential uses.

~~(7)~~~~(8)~~ An observation tower built within Planning Area E-2 and built as part of a large scale recreational use shall not exceed 70 feet.

~~(8)~~(9) All lighting fixtures, including spot lights, electrical reflectors and other means of illumination for signs, structures, landscaping, parking, loading, unloading and similar areas, shall be focused, directed, and arranged to prevent glare or direct illumination on streets or adjoining property.

~~(9)~~(10) Sports lighting, consisting of exterior nighttime lighting for ballfields, racetracks, and other sporting activities, shall not be permitted.

~~(10)~~(11) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.

~~(11)~~(12) Please refer to Section 3.1 – Project Wide Development Standards.

|

**E-3: OPEN SPACE**

Planning Area E-3 provides for the development of 69.33 acres of open space.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards – General**

- (1) Planning Area E-3 is located within Zone B1 of the Jacqueline Cochran Regional Airport CLUP and the ETZ and OSZ airport safety zones. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Large-scale recreational uses such as a motor sports race track are permitted uses in this planning area.
- (3) Planning Area E-3 is accessed from Tyler Street.
- (4) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail** with the exception of the ETZ where no trees shall be planted.
- (5) A Class I Bike Path is located along the north side of Avenue 62 to be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.
- (6) The edge treatment for the interface between the open spaces uses and industrial uses in the adjacent planning area is illustrated in **Figure 3.4-28 Airpark Drainage Buffer**. If the race track is developed, the edge treatment will be as described in **Figure 3.4-24 Golf Course/Race-track Edge Condition**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Project Wide Development Standards.

**E-4: MIXED USED**

Planning Area E-4 provides for 12.25 acres of nonresidential mixed-use land development.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area E-4 is located within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area E-4 accessed from Tyler Street.
- (3) The edge treatment for the interface between the mixed use development in this planning area and the drainage uses in the adjacent planning area is described in **Figure 3.4-28 Airpark Drainage Buffer**.
- (4) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) A Class I Bike Path is located along the north side of Avenue 62 to be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

**E-5: MIXED USE**

Planning Area E-5 provides for 5.31 acres of mixed-use land development with minimum lot sizes of 7,000 square feet as defined in Section 2 – Specific Plan Zoning.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area E-5 is located within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area E-5 accessed from Polk and Tyler Streets.
- (3) The edge treatment for the interface between the mixed use development in this planning area and the drainage uses in the adjacent planning area is described in **Figure 3.4-28 Airpark Drainage Buffer**.
- (4) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) A Class I Bike Path is located along the north side of Avenue 62 to be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.
- (6) Standards for Residential development:
  - a. A maximum of 19 dwelling units with minimum lot sizes of 7,000 square feet, as defined in Section 2-Specific Plan Zoning, may be provided in Planning Area E-5.
  - b. Where a residential lot overlaps two airport land use compatibility zones, the least restrictive land use compatibility zone policy shall apply.
  - c. No buffer is required in Planning Area E-5 for residential uses contiguous to Planning Area E-2.
  - d. Standard Setbacks
    - i. No minimum setback is required from any private street.
    - ii. Front Yard: 30-foot minimum
    - iii. Rear Yard: No minimum
    - iv. Side Yard: 5-feet.
  - e. Residential structures must provide vehicle storage area on the ground floor of structure.
  - f. Access to residential structures shall be from common access areas or corridors.
  - g. Structures on lots with a net area of 7,540 square feet or less shall not exceed the following standards without further ALUC review:
    - i. 7,150 square feet; or
    - ii. two stories or 42 feet in height.
  - h. Structures on lots with a net area greater than 7,540 square feet shall not exceed any of the following standards without further ALUC review:
    - i. Floor Area Ratio – 0.95;
    - ii. Lot Coverage – 0.5; and
    - iii. 42 feet in height.
- (7) All lighting fixtures, including spot lights, electrical reflectors and other means of illumination for signs, structures, landscaping, parking, loading, unloading and similar areas, shall be focused, directed, and arranged to prevent glare or direct illumination on streets or adjoining property.
- (8) Sports lighting, consisting of exterior nighttime lighting for ballfields, racetracks, and other sporting activities, shall not be permitted.
- (9) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (10) Please refer to Section 3.1 – Project Wide Development Standards.

**E-6: MIXED USE**

Planning Area E-6 provides for 33.99 acres of residential and nonresidential mixed-use land development as defined in Section 2 – Specific Plan Zoning.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area E-6 is located within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area E-6 accessed from Polk and Tyler Streets.
- (3) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (4) A Class II Bike Path is located in this planning area along west side of Polk Street. A Class I Bike Path is also located along the north side of Avenue 62 to be developed in accordance with **Figure 3.4-20, Avenue 62 Trail**.
- (5) A Major Project Entry is located at the southeast corner of this planning area to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (6) A total of 8 acres of nonresidential development may occur in the northeastern corner of Planning Area E-6 in addition to residential development.
- (7) Standards for Residential development:
  - a. A maximum of 120 dwelling units with minimum lot sizes of 7,000 square feet, as defined in Section 2-Specific Plan Zoning, may be provided in Planning Area E-6.
  - b. Where a residential lot overlaps two airport land use compatibility zones, the least restrictive land use compatibility zone policy shall apply.
  - c. No buffer is required in Planning Area E-6 for residential uses contiguous to Planning Area E-2.
  - d. Standard Setbacks
    - i. No minimum setback is required from any private street.
    - ii. Front Yard: 30-foot minimum
    - iii. Rear Yard: No minimum
    - iv. Side Yard: 5-feet.
  - e. Residential structures must provide vehicle storage area on the ground floor of structure.
  - f. Access to residential structures shall be from common access areas or corridors.
  - g. Structures on lots with a net area of 7,540 square feet or less shall not exceed the follow standards without further ALUC review:
    - i. 7,150 square feet; or
    - ii. two stories or 42 feet in height.
  - h. Structures on lots with a net area greater than 7,540 square feet shall not exceed any of the following standards without further ALUC review:
    - i. Floor Area Ratio – 0.95;
    - ii. Lot Coverage – 0.5; and
    - iii. 42 feet in height.
- (8) All lighting fixtures, including spot lights, electrical reflectors and other means of illumination for signs, structures, landscaping, parking, loading, unloading and similar areas, shall be focused, directed, and arranged to prevent glare or direct illumination on streets or adjoining property.
- (9) Sports lighting, consisting of exterior nighttime lighting for ballfields, racetracks, and other sporting activities, shall not be permitted.
- (10) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (11) Please refer to Section 3.1 – Project Wide Development Standards.

**E-7: MIXED USE**

Planning Area E-7 provides for 1.32 acres of mixed-use land development.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area E-7 is located within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area E-7 accessed from Polk and Tyler Streets.
- (3) Standards for Residential development:
  - a. A maximum of 5 dwelling units with minimum lot sizes of 7,000 square feet, as defined in Section 2-Specific Plan Zoning, may be provided in Planning Area E-7.
  - b. Where a residential lot overlaps two airport land use compatibility zones, the least restrictive land use compatibility zone policy shall apply.
  - c. No buffer is required in Planning Area E-7 for residential uses contiguous to Planning Area E-2.
  - d. Standard Setbacks
    - i. No minimum setback is required from any private street.
    - ii. Front Yard: 30-foot minimum
    - iii. Rear Yard: No minimum
    - iv. Side Yard: 5-feet.
  - e. Residential structures must provide vehicle storage area on the ground floor of structure.
  - f. Access to residential structures shall be from common access areas or corridors
  - g. Structures on lots with a net area of 7,540 square feet or less shall not exceed the following standards without further ALUC review:
    - i. 7,150 square feet; or
    - ii. two stories or 42 feet in height.
  - h. Structures on lots with a net area greater than 7,540 square feet shall not exceed any of the following standards without further ALUC review:
    - i. Floor Area Ratio – 0.95;
    - ii. Lot Coverage – 0.5; and
    - iii. 42 feet in height.
- (4) Please refer to Section 3.3 – Design and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

**E-8: MIXED USE**

Planning Area E-8 provides for 4.94 acres of mixed-use land development.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area E-8 is located within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area E-8 accessed from Polk and Tyler Streets.
- (3) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (4) Standards for Residential development:
  - a. A maximum of 15 dwelling units with minimum lot sizes of 7,000 square feet, as defined in Section 2-Specific Plan Zoning, may be provided in Planning Area E-8.
  - b. Where a residential lot overlaps two airport land use compatibility zones, the least restrictive land use compatibility zone policy shall apply.
  - c. No buffer is required in Planning Area E-8 for residential uses contiguous to Planning Area E-2.
  - d. Standard Setbacks
    - i. No minimum setback is required from any private street.
    - ii. Front Yard: 30-foot minimum
    - iii. Rear Yard: No minimum
    - iv. Side Yard: 5-feet.
  - e. Residential structures must provide vehicle storage area on the ground floor of structure.
  - f. Access to residential structures shall be from common access areas or corridors
  - g. Structures on lots with a net area of 7,540 square feet or less shall not exceed the following standards without further ALUC review:
    - i. 7,150 square feet; or
    - ii. two stories or 42 feet in height.
  - h. Structures on lots with a net area greater than 7,540 square feet shall not exceed any of the following standards without further ALUC review:
    - i. Floor Area Ratio – 0.95;
    - ii. Lot Coverage – 0.5; and
    - iii. 42 feet in height.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards



### 3.2.6 Neighborhood F

Neighborhood F totals 157.81 acres, and is located along Polk Street, just south of Avenue 62 as depicted in **Figure 3.2-7, Neighborhood F**. It consists of 71.64 acres of medium density residential, 79.94 acres of medium high density residential, and 6.23 acres of commercial retail land use designations. Primary access is provided by "D" Street, Avenue 62 and Polk Street. It is anticipated that the planned commercial land uses will provide employment to project area residents and will service an area larger than the site itself. A Major Entry is located at the intersection of Polk Street and Avenue 62.

#### NEIGHBORHOOD F PLANNING STANDARDS

- (1) Portions of the commercial land uses in Neighborhood F are constrained by the OSZ and ERC airport safety zones as reflected in **Figure 3.1-13, Airport Zones**. Additionally, the neighborhood is located within zones B1, C and D of the CLUP as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to these zones are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) Major project entry is located at the intersection of Avenue 62 and Polk Street to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (3) An intersection detail is planned for "D" Street and Avenue 62 to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (4) "D" Street will provide access to Planning Areas F-4, F-2 and F-3.
- (5) Avenue 62 provides access to Planning Area F-3 and F-4.
- (6) Polk Street provides access to Planning Area F-3.
- (7) Polk Street will be developed in accordance with **Figures 3.4-9, Arterial Streetscape 2 – Polk Street Streetscape 2A**.
- (8) "D" Street will be developed in accordance with **Figure 3.4-18, Collector Streetscape 2**.
- (9) Avenue 62 will be developed in accordance with **Figure 3.4-6, Expressway Streetscape**.

Figure 3.2-7, Neighborhood F

**NEIGHBORHOOD F PLANNING AREAS****F-2: MEDIUM DENSITY RESIDENTIAL**

Planning Area F-2 provides for the development of 71.64 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 under cluster development. A target of 158 dwelling units is planned within a density range of 2.0–5.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area F-2 is located within Zones B1, C and D of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area F-2 is accessed from “D” Street.
- (3) The edge treatment for the interface between the planning area and adjacent drainage channel is shown in **Figure 3.4-27, Channel Buffer Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

**F-3: MEDIUM HIGH DENSITY RESIDENTIAL**

Planning Area F-3 provides for the development of 79.94 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development (Section 2 – Specific Plan Zoning). A target of 635 dwelling units is planned within a density range of 5.0–8.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area F-3 is located within Zones C and D of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area F-3 is accessed from “D” Street, Avenue 62 and Polk Street.
- (3) A Major Project Entry is located in the northeast corner of this planning area to be developed in accordance with **Figure 3.4-3, Major Entry**.
- (4) A Minor Entry is located in the southeast corner of the planning area to be developed in accordance with **Figure 3.4-4, Minor Entry**.
- (5) An intersection detail is located in the northwest corner of this planning area to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards

**F-4: COMMERCIAL RETAIL**

Planning Area F-4 provides for the development of 6.23 acres of commercial land uses.

***Land Use and Development Standards***

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

***Planning Standards***

- (1) Planning Area F-4 is located within Zone B1 and C of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area F-4 is accessed from Avenue 62 and “D” Street.
- (3) An intersection detail is located in the northeast corner of this planning area to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

### 3.2.7 Neighborhood G

Neighborhood G is a predominantly residential neighborhood located roughly in the center of the project site, just east of Tyler Street as depicted in **Figure 3.2-8, Neighborhood F**. Primary access to the neighborhood is provided by "C" Street and Avenue 62. This is a self-contained, inward-focused community that is physically separated from the airport and major circulation routes. A small neighborhood, commercial retail area (4.43 acres) at the northern end of the neighborhood is intended to serve residents of Neighborhood G as well as Neighborhoods C and H. Residential uses include: High Density Residential (20.60 acres), Medium High Density Residential (60.38 acres) and Medium Density Residential (94.20 acres). It is envisioned that this 213.01-acre neighborhood will be a retirement/second home community with a target of 943 dwelling units. Airport safety zones impacting the neighborhood dictate the location of medium density residential uses to the east of "C" Street and discourage the siting of public facilities within the neighborhood. Consequently, the neighborhood is served by public uses to the south in Neighborhood I, located south of Avenue 64. Open space totals 33.40 acres, functioning as part of the storm drainage system and enhanced by windrow plantings, provides a buffer from adjacent neighborhoods and off-site uses. A neighborhood park is located along the eastern edge of the neighborhood, serving Neighborhood H as well. A component of the project-wide trail system is integrated with a power line easement along the neighborhood's southern edge. This trail connects local parks to the north with public facilities to the south.

#### NEIGHBORHOOD G PLANNING STANDARDS

- (1) Portions of the residential and open space land uses in Neighborhood G are constrained by the OSZ, ERC and ETZ airport zones as reflected in **Figure 3.1-13, Airport Zones**. Additionally, the neighborhood is located within Zones B1, C and D of the CLUP for the airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to these zones are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) "C" Street provides access to Planning Areas G-1, G-4, G-5, G-6, G-8, G-10, G-11 and G-13.
- (3) Avenue 62 provides access to Planning Area G-1, G-2, G-3 and G-6.
- (4) The interior east-west loop road provides access to Planning Areas G-7, G-8, G-9, G-10, G-11, G-12 and G-13.
- (5) Avenue 62 will be developed in accordance with **Figure 3.4-6, Expressway Streetscape**, with the exception of the ETZ in which no trees shall be planted.
- (6) "C" Street will be developed in accordance with **Figure 3.4-13, Secondary Streetscape 1**.
- (7) The interior loop road will be developed in accordance with **Figure 3.4-18, Collector Streetscape 2**.
- (8) Windrow plantings will be provided as a buffer between land uses and along drainage channels, and will be integrated into the trail system.
- (9) An intersection detail is planned for "C" Street and Avenue 62 to be developed in accordance with **Figure 3.4-5, Intersection Detail**.

**Figure 3.2-8, Neighborhood G**

**NEIGHBORHOOD G PLANNING AREAS****G-1: COMMERCIAL RETAIL**

Planning Area G-1 provides for the development of 4.43 acres of neighborhood commercial land uses.

***Land Use and Development Standards***

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

***Planning Standards***

- (1) Planning Area G-1 is located within Zone B1 of the CLUP for the Jacqueline Cochran Regional Airport and the OSZ airport safety zone. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-1 is accessed from Avenue 62 and “C” Street.
- (3) The edge treatment between the commercial and adjacent land uses is illustrated in **Figure 3.4-31, Commercial Buffer Streetscape**.
- (4) An intersection detail is located in the northwest corner of this planning area to be developed in accordance with **Figures 3.4-5, Intersection Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

**G-2: OPEN SPACE**

Planning Area G-2 provides for 1.44 acres of open space that will be developed as a local park.

***Land Use and Development Standards***

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

***Planning Standards***

- (1) Planning Area G-2 is located within Zone B1 of the Jacqueline Cochran Regional Airport CLUP and ETZ airport safety zone. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-2 will be developed as a local park.
- (3) Planning Area G-2 is accessed from Avenue 62.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

**G-3: OPEN SPACE**

Planning Area G-3 provides of 1.19 acres of open space that will be developed as a local park.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area G-3 is located within Zone B1 of the Jacqueline Cochran Regional Airport CLUP and ETZ airport safety zone. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-3 will be developed as a local park.
- (3) Planning Area G-3 is accessed from Avenue 62.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

**G-4: OPEN SPACE**

Planning Area G-4 provides for the development of 5.60 acres of open space that will be part of the drainage system for the site.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area G-4 is located within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area G-4 is accessed from “C” Street.
- (3) An intersection detail is located in the northeast corner of this planning area to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (4) This planning area will be developed with the cross-section shown in **Figure 3.4-22, Project Edge**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.



**G-5: MEDIUM DENSITY RESIDENTIAL**

Planning Area G-5 provides for the development of 34.60 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 for cluster development as defined in Section 2 – Specific Plan Zoning Ordinance. A target of 69 dwelling units is planned within a density range of 2.0–5.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) The northern half of Planning Area G-5 is within Zone B1 of the CLUP for the Jacqueline Cochran Regional Airport and the ERC airport safety zone. The southern half is located with Zone C of the CLUP. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-5 is accessed from “C” Street.
- (3) The edge treatment between the residential land use and the adjacent commercial uses to the north is illustrated in **Figure 3.4-29, Out Parcel/Adjacent Property Buffer**.
- (4) The edge treatment between the residential land use and the adjacent drainage channel is shown in **Figure 3.4-27, Channel Buffer Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

**G-6: OPEN SPACE**

Planning Area G-6 provides for the development of 13.65 acres of open space.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area G-6 is within the Jacqueline Cochran Regional Airport ETZ and ERC airport safety zones and Zone C of the CLUP. Development within these zones will comply with all applicable restrictions and requirements.
- (2) The northern most portion of the planning area (1.37 acres) will be developed as a local linear park.
- (3) The remainder of this planning area (12.28 acres) will be developed as a graded drainage channel, with a trail provided to connect with local parks to the north and to the south.
- (4) Planning Area G-6 is accessed from Avenue 62 and “C” Street.
- (5) This Planning Area will be developed with the channel cross-section described in **Figure 3.4-27, Channel Buffer Detail**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

**G-7: MEDIUM HIGH DENSITY RESIDENTIAL**

Planning Area G-7 provides for the development of 60.38 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet for cluster development as defined in Section 2 – Specific Plan Zoning. A target of 432 dwelling units is planned within a density range of 5.0–8.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area G-7 is within Zones C and D of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-7 is accessed from the interior east-west loop road.
- (3) The edge treatment for the interface between this planning area and the adjacent Evacuation Channel is shown in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (4) The Project Edge Condition for this planning area adjacent to Tyler Street is shown in **Figure 3.4-22, Project Edge**.
- (5) The Project Edge Condition for this planning area and adjacent properties is shown in **Figure 3.4-29, out Parcel/Adjacent Property Buffer**.
- (6) Planning Area G-7 will be developed with a windrow/trail detail, as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (7) The southern edge of this planning area will be developed with a trail that will be integrated with the power line easement that runs east-west through the neighborhood as depicted in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (8) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (9) Please refer to Section 3.1 – Project Wide Development Standards.

**G-8: HIGH DENSITY RESIDENTIAL**

Planning Area G-8 provides for the development of 20.60 acres of high density residential land uses with minimum lot sizes of 4,000 square feet under conventional lots and 2,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 282 dwelling units is planned within a density range of 8.0–14.0 du/acre. The maximum density and the total number of dwelling units within the high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area G-8 is within Zones C and D of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-8 is accessed from the interior east-west loop road.
- (3) Planning Area G-8 will be developed with a windrow/trail detail, as shown in **Figure 3.4-21, Windrow/Trail Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

**G-9: OPEN SPACE**

Planning Area G-9 provides of 6.17 acres of open space to be developed as part of the project drainage system.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area G-9 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area G-9 is accessed from the interior east-west loop road.
- (3) The project edge condition for this planning area is indicated in **Figure 3.4-22, Project Edge**.
- (4) The southern edge of this planning area will be developed with a trail that will be integrated with the power line easement that runs east-west through the neighborhood as depicted in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (5) The edge treatment for the interface between this planning area and the adjacent Evacuation Channel is illustrated in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

**G-10: MEDIUM DENSITY RESIDENTIAL**

Planning Area G-10 provides for the development of 18.57 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 under cluster development. A target of 37 dwelling units is planned within a density range of 2.0–5.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area G-10 is within the Jacqueline Cochran Regional Airport ERC and Zone C of the CLUP for the airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-10 is accessed from “C” Street and the interior east-west loop road.
- (3) The edge treatment for the interface between the residential uses and adjacent drainage facilities is illustrated in **Figure 3.4-27, Channel Buffer Detail**.
- (4) The trail along the eastern edge of the planning area will be developed as described in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

**G-11: MEDIUM DENSITY RESIDENTIAL**

Planning Area G-11 provides for the development of 26.86 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 70 dwelling units is planned within a density range of 2.0–5.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area G-11 is within Zone C of the CLUP for the Jacqueline Cochran Regional Airport and the ERC airport safety zone. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-11 is accessed from the interior east-west loop road.
- (3) The edge treatment for the interface with the adjacent park will be as described in **Figure 3.4-25, Park Buffers**.
- (4) Planning Area G-11 will be developed with a windrow/trail detail, as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

**G-12: OPEN SPACE**

Planning Area G-12 provides for 5.35 acres of open space to be developed as a local park.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area G-12 is within Zone C of the CLUP for the Jacqueline Cochran Regional Airport ERC and. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-12 will be developed as a local park.
- (3) Planning Area G-12 is accessed from the interior east-west loop road.
- (4) The park buffer between adjacent residential uses is illustrated in **Figure 3.4-25, Park Buffers**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

**G-13: MEDIUM DENSITY RESIDENTIAL**

Planning Area G-13 provides for the development of 14.17 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 53 dwelling units is planned within a density range of 2.0–5.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area G-13 is within Zone C of the CLUP for the Jacqueline Cochran Regional Airport and the ERC airport safety zone. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area G-13 is accessed from “C” Street and the interior east-west loop road.
- (3) The edge treatment for the interface with the adjacent Evacuation Channel is illustrated in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (4) The southern edge of this planning area will be developed with a trail that will be integrated with the power line easement that runs east-west through the neighborhood as depicted in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

### 3.2.8 Neighborhood H

Neighborhood H, like Neighborhood G, is a residential neighborhood located in the central portion of the site as depicted in **Figure 3.2-9, Neighborhood H**. It is bounded by Polk Street on the east and the Avenue 64 Evacuation Channel to the south. The neighborhood is accessed from Polk Street and "C" Street. It is separated from the residential uses to the north and off-site uses to the east by a landscaped buffer. Residential uses include: High Density Residential (13.55 acres), Medium High Density Residential (69.87 acres) and Medium Density Residential (20.03 acres). A target of 710 dwelling units is planned in this 127.75-acre retirement/second home community. The maximum density and the total number of dwelling units within the high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code.

Open space totals 24.3 acres, functioning as part of the storm drainage system, and enhanced by windrow plantings, and provides a buffer from adjacent neighborhoods and off-site uses. A neighborhood park is located along the western edge of Neighborhood H, serving Neighborhood G as well. A component of the project-wide trail system is integrated with a power line easement along the neighborhood's southern edge. This trail connects local parks to the north with public facilities to the south.

#### NEIGHBORHOOD H PLANNING STANDARDS

- (1) The neighborhood is within Zones C and D of the CLUP for the Jacqueline Cochran Regional Airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to these zones are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) The interior loop road provides access to Planning Areas H-1, H-2, H-3, H-4, H-5, H-6, H-7, H-8, H-9 and H-10.
- (3) Polk Street provides access to Planning Areas H-6 and H-10.
- (4) The interior east-west loop road will be developed in accordance with **Figure 3.4-18, Collector Streetscape 2**.
- (5) The interior north-south loop road will be developed in accordance with **Figure 3.4-18, Collector Streetscape 2**.
- (6) The project entry from Polk Street will be developed in accordance with **Figure 3.4-16, Industrial Collector**.
- (7) A minor project entry is located at the intersection of Polk Street and the entry to the north-south loop road to be developed in accordance with **Figure 3.4-4, Minor Entry**.
- (8) Polk Street will be developed in accordance with **Figure 3.4-10, Arterial Streetscape 2 – Polk Street Streetscape 2B**.
- (9) Windrow plantings will be provided as a buffer between land uses and along drainage channels, and will be integrated into the trail system.

Figure 3.2-9, Neighborhood H

**NEIGHBORHOOD H PLANNING AREAS****H-1: OPEN SPACE**

Planning Area H-1 provides for the development of 5.12 acres of open space.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area H-1 is within Zone C and D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-1 is accessed from the interior east-west loop road, through Planning Areas H-2 and H-6.
- (3) This planning area will be developed with the channel cross-section illustrated in **Figure 3.4-27, Channel Buffer Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

**H-2: MEDIUM HIGH DENSITY RESIDENTIAL**

Planning Area H-2 provides for the development of 18.65 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development (Section 2 – Specific Plan Zoning). A target of 118 dwelling units is planned within a density range of 5.0–8.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area H-2 is within Zones C and of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-2 is accessed from the interior east-west loop road.
- (3) The trail along the western edge of the planning area will be developed as described in **Figure 3.4-21, Windrow/Trail Detail**.
- (4) The edge treatment for the interface between the planning area and adjacent drainage channel is illustrated in **Figure 3.4-27, Channel Buffer Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.



**H-3: OPEN SPACE**

Planning Area H-3 provides for 3.79 acres of open space to be developed as a local park.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area H-3 is within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-3 will be developed as a local park.
- (3) Planning Area H-3 is accessed from the interior east-west loop road.
- (4) The edge treatment for this planning area will be as described in **Figure 3.4-25, Park Buffers**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

**H-4: MEDIUM HIGH DENSITY RESIDENTIAL**

Planning Area H-4 provides for the development of 22.40 acres of medium density residential land uses with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 141 dwelling units is planned within a density range of 5.0–8.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area H-4 is within Zones C and D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-4 is accessed from the interior east-west loop road.
- (3) The edge treatment for this interface between the planning area and the adjacent local park will be as illustrated in **Figure 3.4-25, Park Buffers**.
- (4) Planning Area H-4 will be developed with a buffer between the medium density residential uses in the planning area and the high density residential uses in the adjacent planning area as depicted in **Figure 3.4-32, Product Type Buffers**.
- (5) This planning area will be developed with a windrow/trail detail, as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

**H-5: MEDIUM HIGH DENSITY RESIDENTIAL**

Planning Area H-5 provides for the development of 7.83 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet under conventional development and 4,000 under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 45 dwelling units is planned within a density range of 5.0–8.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area H-5 is within Zone C of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-5 is accessed from the interior east-west and north-south loop roads.
- (3) The trail along the western edge of the planning area will be as shown in **Figure 3.4-21, Windrow/Trail Detail**.
- (4) The edge treatment for the interface between the planning area residential use and the Evacuation Channel is illustrated in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (5) The southern edge of this planning area will be developed with a trail that will be integrated with the power line easement that runs east-west through the neighborhood as depicted in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

**H-6: OPEN SPACE**

Planning Area H-6 provides for the development of 11.43 acres of open space which will be part of the project drainage network and trail system.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area H-6 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-6 will be developed to incorporate an element of the project-wide trail system.
- (3) Planning Area H-6 is accessed from Polk Street and the interior east-west loop road.
- (4) This planning area will be developed with the cross-section shown in **Figure 3.4-10, Arterial Streetscape 2/Polk Street Streetscape 2B**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

**H-7: MEDIUM HIGH DENSITY RESIDENTIAL**

Planning Area H-7 provides for the development of 20.99 acres of medium high density residential land uses with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 151 dwelling units is planned within a density range of 5.0–8.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area H-7 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-7 is accessed from the interior east-west loop road.
- (3) The edge treatment for the interface between the planning area and adjacent drainage channel is illustrated in **Figure 3.4-10, Arterial Streetscape 2/Polk Street Streetscape 2B**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

**H-8: HIGH DENSITY RESIDENTIAL**

Planning Area H-8 provides for the development of 13.55 acres of high density residential land uses with minimum lot sizes of 4,000 square feet under conventional lots and 2,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 189 dwelling units is planned within a density range of 8.0–14.0 du/acre. The maximum density and the total number of dwelling units within the high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area H-8 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-8 is accessed from the interior east-west loop road.
- (3) Planning Area H-8 will be developed with a buffer between the high density residential uses in the planning area, and the medium density residential uses in the adjacent planning area as depicted in **Figure 3.4-32, Product Type Buffer**.
- (4) This planning area will be developed with a windrow/trail detail, as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

**H-9: MEDIUM DENSITY RESIDENTIAL**

Planning Area H-9 provides for the development of 20.03 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 66 dwelling units is planned within a density range of 2.0–5.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area H-9 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-9 is accessed from the interior east-west loop road.
- (3) The edge treatment for the interface with the planning area and the adjacent drainage channel is illustrated in **Figure 3.4-10, Arterial Streetscape 2/Polk Streetscape 2B**.
- (4) The edge treatment for the interface with the Evacuation Channel is shown in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (5) The southern edge of this planning area will be developed with a trail that will be integrated with the power line easement that runs east-west through the neighborhood as depicted in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

**H-10: OPEN SPACE**

Planning Area H-10 provides for the development of 3.96 acres of open space as part of the project drainage network and trail system.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area H-10 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area H-10 is accessed from Polk Street and the interior east-west loop road.
- (3) The edge treatment between the planning area and adjacent residential uses is depicted in **Figure 3.4-10, Arterial Streetscape 2/Polk Streetscape 2B**.
- (4) The southern edge of this planning area will be developed with a trail that will be integrated with the power line easement that runs east-west through the neighborhood as depicted in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

### 3.2.10 Neighborhood I

Neighborhood I is a predominantly residential neighborhood located just south of the Avenue 64 Evacuation Channel, between Tyler and Polk Streets as depicted in **Figure 3.2-10, Neighborhood I**. This neighborhood contains eleven planning areas comprised of 89.9 acres and is the northernmost neighborhood outside of the airport Zone C. Proposed land uses include Medium Density Residential (75.9 acres) and Open Space to be developed as part of the project storm drainage system (14.0 acres). A target of 338 dwelling units is planned for Neighborhood I. The open space area serves as a land use buffer and part of the drainage network. The neighborhood is accessed from Tyler Street, "E" Street, "F" Street and Avenue 64. Within the Kohl Ranch Circulation Plan, "E" Street has been realigned to circulate north and south between Avenue 64 and Avenue 66, replacing its previously proposed curvilinear location south of the evacuation channel.

#### NEIGHBORHOOD I PLANNING STANDARDS

- (1) The neighborhood is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to these zones are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) A Minor entry is located at the intersection of Tyler Street and "F" Street to be developed in accordance with **Figure 3.4-4, Minor Entry**.
- (3) Primary access to the neighborhood is provided from Tyler Street, "E" Street, "F" Street, Avenue 64 and the interior loop roads.
- (4) "E" Street provides access to Planning Area I-8, I-9, I-10 and I-11 through interior loop roads.
- (5) Avenue 64 provides access to Planning Areas I-1.
- (6) "F" Street provides access to Planning Areas I-2, I-3, I-4, I-5, I-6, I-7 and I-8 through interior loop roads.
- (7) Tyler Street provides access to Planning Area I-1 and I-2.
- (8) "E" Street and Avenue 64 will be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (9) Tyler Street will be developed in accordance with **Figure 3.4-14, Secondary Streetscape 2**.
- (10) The Evacuation Channel in Planning Area I-1 will be developed with the Canal Buffer depicted in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (11) The intersection of Avenue 64 and "E" Street will be developed with the Intersection Detail as shown in **Figures 3.4-5, Intersection Detail**.
- (12) The east side of Tyler Street will be developed with a Class I Bike path as illustrated in **Figure 3.1-10, Public Facilities**.

Figure 3.2-10, Neighborhood I

**NEIGHBORHOOD I PLANNING AREAS****I-1: OPEN SPACE**

Planning Area I-1 provides for the development of 9.6 acres of open space that includes the Avenue 64 Evacuation Channel and land adjacent to the channel.

***Land Use and Development Standards***

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

***Planning Standards***

- (1) Planning Area I-1 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area I-1 is accessed from Avenue 64.
- (3) An Intersection Detail will be located at the northeast corner of the planning area as illustrated in **Figure 3.4-5, Intersection Detail**.
- (4) This planning area will be developed with the Evacuation Channel Buffer for the interface with adjacent residential development, as shown in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (5) Planning Area I-1 will be developed with a windrow/trail detail as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (6) A Class I Bike Path is located in this planning area on the east side of Tyler Street as illustrated in **Figure 3.1-10, Public Facilities**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Project Wide Development Standards.

**I-2: OPEN SPACE**

Planning Area I-2 provides for the development of 3.9 acres of open space to be developed as part of the project storm drainage system.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area I-2 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) This planning area is accessed from Tyler Street and “F” Street.
- (3) This planning area will be developed with the project edge treatment for the interface between the drainage facilities and project boundary, as shown in **Figure 3.4-22, Project Edge**.
- (4) A Minor Project Entry is located at the southwest corner of this planning area to be developed in accordance with **Figure 3.4-4, Minor Entry**.
- (5) A Class I Bike Path is located in this planning area on the east side of Tyler Street as illustrated in **Figure 3.1-10, Public Facilities**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

**I-3: OPEN SPACE**

Planning Area I-3 provides for the development of 0.5 acres of open space that abuts the Avenue 64 Evacuation Channel to be developed as part of the project storm drainage system.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area I-3 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area I-3 is accessed from “F” Street through interior loop roads.
- (3) This planning area will be developed with the drainage channel buffer for the interface with adjacent residential uses, as shown in **Figure 3.4-27, Channel Buffer Detail**.
- (4) The drainage way in Planning Area I-3 will be designed to provide a pedestrian trail that connects with the project-wide trail system as depicted in **Figure 3.4-21 Windrow/Trail Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.



**I-4: MEDIUM DENSITY RESIDENTIAL**

Planning Area I-4 provides for the development of 7.0 acres of medium density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 35 dwelling units is planned within a density range of 2.0–5.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area I-4 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) This planning area is accessed from "F" Street through interior loop roads.
- (3) The buffer between the drainage facilities and adjacent residential uses will be developed as shown in **Figure 3.4-27, Channel Buffer Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

**I-5: MEDIUM DENSITY RESIDENTIAL**

Planning Area I-5 provides for 10.06 acres of medium density residential land uses with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development. A target of 51 dwelling units is planned within a density range of 2.0–5.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area I-5 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area I-5 is accessed from "F" Street through interior loop roads.
- (3) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (4) Please refer to Section 3.1 – Project Wide Development Standards.

**I-6: MEDIUM DENSITY RESIDENTIAL**

Planning Area I-6 provides for the development of 9.6 acres of medium density residential uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 42 dwelling units is planned within a density range 2.0–5.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area I-6 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area I-6 is accessed from “F” Street through the interior loop road.
- (3) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (4) Please refer to Section 3.1 – Project Wide Development Standards.

**I-7: MEDIUM DENSITY RESIDENTIAL**

Planning Area I-7 provides for the development of 6.7 acres of medium density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 29 dwelling units is planned within a density range 2.0–5.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area I-7 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area I-7 is accessed from “F” Street through interior loop roads.
- (3) The buffer between the drainage facilities and adjacent residential uses will be developed as shown in **Figure 3.4-27, Channel Buffer Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

**I-8: MEDIUM DENSITY RESIDENTIAL**

Planning Area I-8 provides for the development of 8.0 acres of medium density residential land uses with minimum lot sizes of 5,000 square feet for conventional lots and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 40 dwelling units is planned within a density range of 2.0–5.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area I-8 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area I-8 is accessed from “E” Street through interior loop roads.
- (3) The buffer between the drainage facilities and adjacent residential uses will be developed as shown in **Figure 3.4-27, Channel Buffer Detail**.
- (4) A project trail will be developed in the easternmost portion of the planning area as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

**I-9: MEDIUM DENSITY RESIDENTIAL**

Planning Area I-9 provides for the development of 11.5 acres of medium density residential uses with minimum lot sizes of 5,000 square feet for conventional development and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 57 dwelling units is planned within a density range 2.0–5.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area I-9 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) This planning area is accessed from “F” Street through interior loop roads.
- (3) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (4) Please refer to Section 3.1 – Project Wide Development Standards.

**I-10: MEDIUM DENSITY RESIDENTIAL**

Planning Area I-10 provides for the development of 12.4 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development. A target of 47 dwelling units is planned within a density range of 2.0–5.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area I-10 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area I-10 is accessed from “E” Street.
- (3) A project trail will be developed in the easternmost portion of the planning area as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

**I-11: MEDIUM DENSITY RESIDENTIAL**

Planning Area I-11 provides for the development of 10.1 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet for cluster development as defined in Section 2 – Specific Plan Zoning. A target of 37 dwelling units is planned within a density range of 2.0–5.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area I-11 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area I-11 is accessed from “E” Street.
- (3) A project trail will be developed in the easternmost portion of the planning area as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

### 3.2.11 Neighborhood J

Neighborhood J is located in the southern section of the project site as depicted in **Figure 3.2-11, Neighborhood J**. The 119.49-acre neighborhood is located between "E" Street and Polk Street, just south of Avenue 64. It contains eight planning areas consisting of the following land uses: Medium Density Residential (80.88 acres) and Open Space (38.61 acres). A target of 319 units is planned for Neighborhood J. Access to the neighborhood is provided from "E" Street and Polk Street. The significant amount of open space within and adjacent to the neighborhood functions as part of the drainage system and provides a scenic amenity and buffer along Polk Street, as well as a local park. If developed with a golf course, a permitted use under provisions of Section 2 – Specific Plan Zoning, Neighborhood J would be equipped with a clubhouse which would provide meeting, dining and recreational opportunities to neighborhood residents.

#### NEIGHBORHOOD J PLANNING STANDARDS

- (1) A golf course use is permitted in all of the planning areas. A clubhouse is permitted in one of the planning areas. Additionally, recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water) are permitted in all of the planning areas.
- (2) The neighborhood is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to these zones are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (3) Primary access to Neighborhood J is from "E" Street, Avenue 64, Polk Street and the interior loop roads.
- (4) "E" Street provides access to Planning Areas J-1, J-6 and J-7.
- (5) Polk Street provides access to Planning Areas J-1 through J-8.
- (6) An Intersection Detail is located at the intersection of Avenue 64 and "E" Street to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (7) A Minor Entry is located at the intersection of Polk Street and the interior east-west loop road to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (8) "E" Street will be developed in accordance with **Figure 3.4-11, Major Streetscape 1**.
- (9) Polk Street will be developed in accordance with **Figure 3.4-9, Arterial Streetscape 2 – Polk Street Streetscape 2A**.
- (10) Avenue 64 will be developed in accordance with **Figure 3.4-11, Major Streetscape 1**.
- (11) The west side of Polk Street will be developed with a Class I Bike path as illustrated in **Figure 3.1-10, Public Facilities**.

Figure 3.2-11, Neighborhood J

**NEIGHBORHOOD J PLANNING AREAS****J-1: MEDIUM DENSITY RESIDENTIAL**

Planning Area J-1 provides for 37.52 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 130 dwelling units is planned for this planning area within a density range of 2.0–5.0 du/ac.

***Land Use and Development Standards***

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

***Planning Standards***

- (1) Planning Area J-1 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area J-1 is accessed from “E” Street and Polk Street.
- (4) This planning area will be developed with the Evacuation Channel Buffer for the interface with adjacent residential development, as shown in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (5) Planning Area J-1 will be developed in accordance with **Figure 3.4-32, Product Type Buffer** between the medium density residential uses within the planning area and the high density residential uses in the adjacent planning area.
- (6) The edge treatment for the interface between the residential uses and adjacent drainage facilities is shown in **Figure 3.4-28, Airpark Drainage Buffer**.
- (7) A Class I Bike Path is located in this planning area on the west side of Polk Street as illustrated in **Figure 3.1-10, Public Facilities**.
- (8) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (9) Please refer to Section 3.1 – Project Wide Development Standards.

**J-2: OPEN SPACE**

Planning Area J-2 provides for the development of 22.0 acres of open space to be developed as a lake and as part of the project drainage network.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area J-2 is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) If the golf course or recreational lakes are not developed, this planning area will be developed with the edge treatment shown in **Figure 3.4-23, Drainage Canal Edge Condition**.
- (4) Planning area J-2 is accessed from “E” Street.
- (5) The edge treatment for the interface between the drainage facilities and adjacent residential uses is shown in **Figure 3.4-23, Drainage Canal Edge Condition**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.



**J-3: OPEN SPACE**

Planning Area J-3 provides for the development of 8.8 acres of open space to be a landscaped perimeter to the lake, containing a walking trail for the use and enjoyment of project residents.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area J-3 is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area J-3 is accessed from “E” Street and the interior loop roads All planning areas abutting the lake will have access to the lake perimeter trail.
- (4) The edge treatment for the interface with the adjacent drainage facilities is shown in **Figure 3.4-23, Drainage Canal Edge Condition.**
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

**J-4: MEDIUM DENSITY RESIDENTIAL**

Planning Area J-4 provides for the development of 43.36 acres of medium density residential land uses with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 189 dwelling units is planned within a density range of 2.0–5.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area J-4 is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Golf course, including clubhouse facilities, and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing, and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area J-4 is accessed from Polk Street.
- (4) This planning area will be developed with the project edge treatment as depicted in **Figure 3.4-24, Golf Course (Racetrack) Edge Condition.**
- (5) A pedestrian trail is located along the eastern boundary of this planning area to be developed in accordance with **Figure 3.4-21, Windrow/Trail Detail.**
- (6) A minor project entry is located in the southeast corner of this planning area to be developed in accordance with **Figure 3.4-4, Minor Entry.**
- (7) If the golf course or recreational lakes are not developed, this planning area will be developed with the edge treatment shown in **Figure 3.4-23, Drainage Canal Edge Condition.**
- (8) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (9) Please refer to Section 3.1 – Project Wide Development Standards.

**J-5: OPEN SPACE**

Planning Area J-5 provides for the development of 1.0 acres of open space to be developed as a local park or open space.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area J-5 is within Zone E of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area J-5 is accessed from Polk Street through interior loop roads.
- (4) The edge treatment for the interface between the park and adjacent residential uses is shown in **Figure 3.4-25, Park Buffers**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

**J-6: OPEN SPACE**

Planning Area J-6 provides for the development of 2.2 acres of open space to be developed as a park or open space that may include a recreation center.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area J-6 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area J-6 is accessed from “E” Street and Polk Street through interior loop roads.
- (4) The edge treatment for the interface between the park and adjacent residential land uses is illustrated in **Figure 3.4-25, Park Buffers**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

**J-7: OPEN SPACE**

Planning Area J-7 provides for the development of 3.3 acres of open space that includes the Avenue 64 Evacuation Channel and land adjacent to the channel.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area J-7 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning area J-7 is accessed from Avenue 64.
- (3) An Intersection Detail is located in the northwest corner of the planning area to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (4) A pedestrian trail will be developed along the eastern boundary of the planning area to be developed in accordance with **Figure 3.4-21, Windrow/Trail Detail**. A Class I Bike Path is located in this planning area on the west side of Polk Street as illustrated in **Figure 3.1-10, Public Facilities**.
- (5) This planning area will be developed with the Evacuation Channel Buffer for the interface with adjacent residential development, as shown in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

**J-8: OPEN SPACE**

Planning Area J-8 provides for the development of 1.31 acres of open space to be developed as part of the project storm drainage system.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area J-8 is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area J-8 is accessed from Polk Street.
- (3) The drainage way in Planning Area J-8 will be designed to provide a pedestrian trail that connects with the project-wide trail system as depicted in **Figure 3.4-21, Windrow/Trail Detail**. A Class I Bike Path is located in this planning area on the west side of Polk Street as illustrated in **Figures 3.1-10, Public Facilities**.
- (4) The edge treatment for the interface with the planning areas drainage facilities and adjacent residential land uses is depicted in **Figure, 3.4-23, Drainage Canal Edge Condition**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

### 3.2.12 Neighborhood K

Neighborhood K is located in the southern portion of the project site as depicted in **Figure 3.2-12, Neighborhood K**. The neighborhood is 34.4 acres of open space with no dwelling units planned. The lake provides a scenic amenity, includes support of open space features such as a local park and recreation center, and functions as the dual water system (irrigation storage) and as part of the project drainage network. Primary access to the neighborhood is provided from “E” Street and “F” Street.

#### NEIGHBORHOOD K PLANNING STANDARDS

- (1) The neighborhood is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to this zone are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) A golf course use or recreational lake (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) is a permitted use in all of the planning areas.
- (3) Primary access to the neighborhood is provided by “E” Street and “F” Street as depicted in **Figure 3.3-1, Community Character**.
- (4) “E” Street provides access to Planning Area K-1 to K-3.
- (5) “F” Street will be developed in accordance with **Figure 3.4-17, Collector Streetscape 1**.
- (6) “E” Street will be developed in accordance with **Figure 3.4-12, Major Streetscape 2**.

Figure 3.2-12, Neighborhood K



**NEIGHBORHOOD K PLANNING AREAS****K-1: Open Space**

Planning Area K-1 provides for the development of 22.4 acres of open space to be developed as a lake for the for the dual water system (irrigation storage), aesthetics and detention as part of the project drainage network.

***Land Use and Development Standards***

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

***Planning Standards***

- (1) Planning Area K-1 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area K-1 is accessed from “E” Street and “F” Street.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

**K-2: Open Space**

Planning Area K-2 provides for 8.8 acres of open space to be a landscaped perimeter to the lake, containing a walking trail for the use and enjoyment of project residents.

***Land Use and Development Standards***

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

***Planning Standards***

- (1) Planning Area K-2 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area K-2 is accessed from “E” Street and “F” Street.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

**K-3: Open Space**

Planning Area K-3 provides for the development of 3.2 acres of open space to be developed as a park.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area K-3 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area K-3 is accessed from “F” Street.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

### 3.2.13 Neighborhood L

Neighborhood L is located in the southern portion of the project site as depicted in **Figure 3.2-13, Neighborhood L**. The 111.4-acre neighborhood is located in the southeast corner of the project site, and is bounded by and accessed from Polk Street on the east, Avenue 66 on the south and "E" Street on the west. Land uses consist of High Density Residential (106.2 acres) and open space (5.2 acres). A target of 1,158 dwelling units is planned for Neighborhood L. The maximum density and the total number of dwelling units within the high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code. Higher density residential uses are located at the project periphery, with lower density uses more interior to the site. If developed with a golf course, a permitted use under the provisions of Section 2 – Specific Plan Zoning, Neighborhood L would be equipped with a significant recreational and visual amenity.

#### NEIGHBORHOOD L PLANNING STANDARDS

- (1) The neighborhood is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to this zone are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) A golf course use and/or recreational lake (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) is a permitted use in all of the planning areas.
- (3) Primary access is taken from "E" Street, Avenue 66 and Polk Street.
- (4) "E" Street provides access to Planning Area L-1
- (5) Avenue 66 provides access to Planning Areas L-2.
- (6) Polk Street provides access to Planning Area L-3.
- (7) A Major Project Entry is located at the intersection of "E" Street and Avenue 66 to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (8) A Minor Entry is located at the intersection of Polk Street with the interior loop road to be developed in accordance with **Figure 3.4-4, Minor Project Entry**.
- (9) Avenue 66 will be developed in accordance with **Figure 3.4-7, Urban Arterial Streetscape**.
- (10) "E" Street will be developed in accordance with **Figure 3.4-12, Major Streetscape 2**.
- (11) Polk Street will be developed in accordance with **Figure 3.4-9, Arterial Streetscape 2 – Polk Street Streetscape 2A**.
- (12) The west side of Polk Street and the north side of Avenue 62 will be developed with a Class I Bike path as illustrated in **Figure 3.1-10, Public Facilities**.

Figure 3.2-13, Neighborhood L

**NEIGHBORHOOD L PLANNING AREAS****L-1: HIGH DENSITY RESIDENTIAL**

Planning Area L-1 provides for the development of 106.2 acres of high density residential land uses with minimum lot sizes of 4,000 square feet under conventional development and 2,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 1,158 dwelling units is planned within a density range of 8.0–14.0 du/acre. The maximum density and the total number of dwelling units within the high density residential designation can be exceeded through the application of density bonuses for affordable housing, as permitted by the provisions of Section 65915 of the California Government Code.

***Land Use and Development Standards***

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

***Planning Standards***

- (1) Planning Area L-1 is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area L-1 is accessed from Avenue 66 and “E” Street.
- (4) The edge treatment for the interface between the residential land use and adjacent park use is shown in **Figure 3.4-25, Park Buffers**.
- (5) The edge treatment for the interface between the residential land uses and adjacent drainage facilities is shown in **Figure 3.4-23, Drainage Canal Edge Condition**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

**L-2: OPEN SPACE**

Planning Area L-2 provides for the development of 4.1 acres of open space to be developed as part of the project storm drainage system.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area L-2 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area L-2 is accessed from Avenue 66.
- (4) A major project entry is located in the southwest corner of the planning area to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (5) The edge treatment for the interface with the planning areas drainage facilities and adjacent residential land use is depicted in **Figure 3.4-28, Airpark Drainage Buffer**.
- (6) The drainage way in Planning Area L-2 will be designed to provide a pedestrian trail that connects with the project-wide trail system in accordance with **Figure 3.4-21, Windrow/Trail Detail**. A Class I Bike Path is located in this planning area on the north side of Avenue 66 as illustrated in **Figure 3.1-10, Public Facilities**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Project Wide Development Standards.

**L-3: OPEN SPACE**

Planning Area L-3 provides for the development of 1.1 acres of open space to be developed as part of the project storm drainage system.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area L-3 is within Zone E of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area L-3 is accessed from Polk Street.
- (4) A major project entry is located in the southwest corner of the planning area to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (5) The drainage way in Planning Area L-3 will be designed to provide a pedestrian trail that connects with the project-wide trail system to be developed in accordance with **Figure 3.4-21, Windrow/Trail Detail**. A Class I Bike Path is located in this planning area on the west side of Polk Street as illustrated in **Figure 3.1-10, Public Facilities**.
- (6) The edge treatment for the interface with the planning areas drainage facilities and adjacent residential land uses is depicted in **Figure 3.4-23, Drainage Canal Edge Condition**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Project Wide Development Standards.

### 3.2.14 Neighborhood M

Neighborhood M is located in the southwest corner of the project site, north of Avenue 66 to "F" Street between Tyler Street and "E" Street as depicted in **Figure 3.2-14, Neighborhood M**. This 219.8-acre neighborhood contains Medium Density Residential (78.90 acres) and Medium High Density Residential (47.50 acres). A target of 555 dwelling units is planned for Neighborhood M. This neighborhood also includes 84.3 acres of Public Facilities/Schools. Open Space totals 9.1 acres and functions as part of the drainage network, provides a buffer along Tyler Street, and provides a buffer along the residential interface with the school site. Under a golf course scenario and recreational lake (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) scenario, open space would provide active recreational benefits as well. Primary access to Neighborhood M is from Tyler Street, "E" Street, Avenue 66 and "F" Street.

#### NEIGHBORHOOD M PLANNING STANDARDS

- (1) The neighborhood is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport as reflected in **Figure 3.1-14, Airport Land Use Compatibility Zones and Utility Easement Beltway**. The development restrictions which apply to this zone are described in Section 3.1.10 – Airport Noise and Utility Easement Beltway and **Table 3.1-G, Basic Compatibility Criteria** of this Specific Plan.
- (2) A golf course and/or recreational lake (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in all of the planning areas.
- (3) Primary access is provided by Tyler Street, Avenue 66, "E" Street and "F" Street.
- (4) A Major Project Entry is located at the intersection of "E" Street and Avenue 66 to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (5) A Minor Project Entry is located at "F" Street and Tyler Street to be developed in accordance with **Figure 3.4-4, Minor Project Entry**.
- (6) "F" Street provides access to Planning Areas M-1A, M-1B, M-1C, M-5A, M-5B, M-6A, M-6B and M-7A.
- (7) The intersection of "F" Street and "E" Street will be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (8) "F" Street, just east of the Minor Project Entry on Tyler Street to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (9) "E" Street provides access to Planning Areas M-6B, M-7C, M-7D, M-7E and M-9.
- (10) "E" Street will be developed in accordance with **Figure 3.4-11, Major Streetscape 1**.
- (11) "F" Street will be developed in accordance with **Figure 3.4-17, Collector Streetscape 1**.
- (12) Avenue 66 will be developed in accordance with **Figure 3.4-7, Urban Arterial Streetscape**.
- (13) Tyler Street, south of "F" Street to the boundary of Planning Area M-2 will be developed in accordance with **Figure 3.4-15, Modified Secondary Streetscape 2**. Tyler Street, south of Planning Area M-2 to Avenue 66 will be developed in accordance with **Figure 3.4-17, Collector Streetscape 1**.
- (14) The north side of Avenue 62 will be developed with a Class I Bike path as illustrated in **Figure 3.1-10, Public Facilities**.



**Figure 3.2-14, Neighborhood M**

**NEIGHBORHOOD M PLANNING AREAS****M-1A: Medium Density Residential**

Planning Area M-1A provides for the development of 2.5 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 8 dwelling units is planned within a density range of 2.0–5.0 du/acre.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area M-1A is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area M-1A is accessed from "F" Street.
- (4) The edge treatment for the interface between the residential uses and the lake uses in the adjacent planning area is depicted in **Figure 3.4-23, Drainage Canal Edge Condition**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

**M-1B: Medium Density Residential**

Planning Area M-1B provides for the development of 18.9 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development. A target of 68 dwelling units is planned within a density range 2.0–5.0 du/ac.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area M-1B is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area M-1B is accessed from “F” Street.
- (3) The buffer between the Planning Area M-1B’s residential uses and the adjacent lake facilities and will be developed as shown in **Figure 3.4-27, Channel Buffer Detail**.
- (4) An intersection detail is located in the northeast corner of this planning area to be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (5) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (6) Please refer to Section 3.1 – Project Wide Development Standards.

**M-1C: Medium Density Residential**

Planning Area M-1C provides for the development of 16.4 acres of medium density residential land uses with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 61 dwelling units is planned within a density range 2.0–5.0 du/ac.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area M-1C is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Planning Area M-1C is accessed from “F” Street.
- (3) The buffer between the Planning Area M-1C’s residential uses and the adjacent lake facilities and will be developed in accordance with **Figure 3.4-23, Drainage Canal Edge Condition**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

**M-2: Open Space**

Planning Area M-2 provides for the development of 1.9 acres of open space to be developed as a land use buffer and as part of the project storm drainage system.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area M-2 is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area M-2 is accessed from Tyler Street.
- (4) The northwest corner of this planning area will be developed in accordance with a **Figure 3.4-4, Minor Project Entry**.
- (5) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (6) The buffer between the Planning Area M-2's drainage facilities and the adjacent residential uses will be developed as shown in **Figure 3.4-23, Drainage Canal Edge Condition**.
- (7) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (8) Please refer to Section 3.1 – Project Wide Development Standards.

**M-3: Open Space**

Planning Area M-3 provides for the development of 1.60 acres of open space to contain part of the southern drainage channel.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area M-3 is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (2) Golf course and/or recreational lakes (lakes, including those used for aesthetics, detention, recreation, water skiing and non-potable irrigation water) are permitted uses in this planning area. Lakes shall incorporate the design guidelines set forth in Section 3.1.10 – Airport Zones and Utility Easement Beltway for Airport Compatibility Design Guidelines for Lakes.
- (3) Planning Area M-3 is accessed through the loop circulation system within Planning Area M-1B, from “F” Street.
- (4) This planning area will be developed with the edge conditions for the interface between drainage facilities and adjacent residential land uses, as shown in **Figure 3.4-23, Drainage Canal Edge Condition**.
- (5) The Windrow/Trail Detail for this planning area will be developed as illustrated in **Figure 3.4-21, Windrow/Trail Detail**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

**M-4: Public Facilities/Schools**

Planning Area M-4 is an existing 84.30-acre public facilities/schools site owned by the Coachella Valley Unified School District housing the Las Palmitas Elementary School, Toro Canyon Middle School, and Desert Mirage High School.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area M-4 is accessed from Tyler Street and Avenue 66.
- (2) Planning Area M-4 is within Zones D and E of the CLUP for the Jacqueline Cochran Regional Airport. Development within these zones will comply with all applicable restrictions and requirements.
- (3) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (4) Please refer to Section 3.1 – Project Wide Development Standards.

**M-5A: Medium Density Residential**

Planning Area M-5A provides for the development of 4.1 acres of medium density residential land use with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 14 dwelling units is planned within a density range of 2.0–5.0 du/ac.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area M-5A is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-5A is accessed from “F” Street.
- (3) This planning area will be developed in accordance with **Figure 3.4-25, Park Buffers** between planning areas residential and adjacent park uses.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

**M-5B: Medium Density Residential**

Planning Area M-5B provides for the development of 15.7 acres of medium density residential land use with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 61 dwelling units is planned within a density range of 2.0–5.0 du/ac.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area M-5B is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-5B is accessed from “F” Street.
- (3) This planning area will be developed in accordance with **Figure 3.4-23, Drainage Canal Edge Condition** for the interface with adjacent drainage facilities.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

**M-6A: Medium Density Residential**

Planning Area M-6A provides for the development of 3.8 acres of medium density residential land use with minimum lot sizes of 6,000 square feet under conventional development and 5,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 14 dwelling units is planned within a density range of 2.0–5.0 du/ac.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area M-6A is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-6A is accessed from “F” Street.
- (3) This planning area will be developed with in accordance with **Figure 3.4-25, Park Buffers** between residential land and adjacent park uses.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

**M-6B: Medium Density Residential**

Planning Area M-6B provides for the development of 13.4 acres of medium density residential land use with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet for cluster development as defined in Section 2 – Specific Plan Zoning. A target of 57 dwelling units is planned within a density range of 2.0–5.0 du/ac.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area M-6B is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-6B is accessed from “E” Street and “F” Street.
- (3) The northeast corner of this planning area will be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

**M-7A: Medium Density Residential**

Planning Area M-7A provides for the development of 4.1 acres of medium density residential land use with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 20 dwelling units is planned within a density range of 2.0–5.0 du/ac.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area M-7A is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-7A is accessed from “E” Street and “F” Street.
- (3) The southeast corner of this planning area will be developed in accordance with **Figure 3.4-5, Intersection Detail**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

**M-7B: Medium High Density Residential**

Planning Area M-7B provides for the development of 11.0 acres of medium high density residential land use with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development (Section 2 – Specific Plan Zoning). A target of 56 dwelling units is planned within a density range of 5.0–8.0 du/ac.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area M-7B is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-7B is accessed from “E” Street through interior loop roads.
- (3) This planning area will be developed with the edge conditions for the interface with adjacent drainage facilities, as shown in **Figure 3.23, Drainage Canal Edge Conditions**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.



**M-7C: Medium High Density Residential**

Planning Area M-7C provides for the development of 5.9 acres of medium high density residential land use with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 32 dwelling units is planned within a density range of 5.0–8.0 du/ac.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area M-7C is within Zone E of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-7C is accessed from “E” Street.
- (3) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (4) Please refer to Section 3.1 – Project Wide Development Standards.

**M-7D: Medium High Density Residential**

Planning Area M-7D provides for the development of 15.1 acres of medium high density residential land use with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 82 dwelling units is planned within a density range of 5.0–8.0 du/ac.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area M-7D is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-7D is accessed from “E” Street.
- (3) This planning area will be developed with the edge conditions for the interface with adjacent drainage facilities, as shown in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

**M-7E: Medium High Density Residential**

Planning Area M-7E provides for the development of 15.5 acres of medium high density residential land use with minimum lot sizes of 5,000 square feet under conventional development and 4,000 square feet under cluster development as defined in Section 2 – Specific Plan Zoning. A target of 82 dwelling units is planned within a density range of 5.0–8.0 du/ac.

*Land Use and Development Standards*

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area M-7E is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-7E is accessed from “E” Street.
- (3) This planning area will be developed with the edge conditions for the interface with adjacent drainage facilities, as shown in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

**M-8: Open Space**

Planning Area M-8 provides for the development of 2.9 acres of open space to be developed as a land use buffer and as part of the project storm drainage system.

*Land Use and Development Standards*

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area M-8 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-8 is accessed from the loop circulation road within Planning Area M-5B and from the loop circulation road within Planning Area M-7B via “E” Street.
- (3) This planning area will be developed with the edge conditions for the interface with adjacent drainage facilities, as shown in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (4) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (5) Please refer to Section 3.1 – Project Wide Development Standards.

**M-9: Open Space**

Planning Area M-9 provides for the development of 2.7 acres of open space to be developed as a land use buffer and as part of the project storm drainage system.

**Land Use and Development Standards**

Please refer to Ordinance No. 348 (Section 2 – Specific Plan Zoning Ordinance).

**Planning Standards**

- (1) Planning Area M-9 is within Zone D of the CLUP for the Jacqueline Cochran Regional Airport. Development within this zone will comply with all applicable restrictions and requirements.
- (2) Planning Area M-9 is accessed from the intersection of Avenue 66 and “E” Street.
- (3) This planning area will be developed with the edge conditions for the interface with the drainage facilities and adjacent residential land use, as shown in **Figure 3.4-23, Drainage Canal Edge Conditions**.
- (4) A Major Project Entry is located in the southeast corner of the planning area to be developed in accordance with **Figure 3.4-3, Major Project Entry**.
- (5) The drainage way in Planning Area M-9 will be designed to provide a pedestrian trail that connects with the project-wide trail system to be developed in accordance with **Figure 3.4-21, Windrow/Trail Detail**. A Class I Bike Path is located in this planning area on the north side of Avenue 66 as illustrated in **Figures 3.1-10, Public Facilities**.
- (6) Please refer to Section 3.3 – Design Guidelines and Section 3.4 – Landscape Design Guidelines for specific and other related design criteria.
- (7) Please refer to Section 3.1 – Project Wide Development Standards.

## 3.3 DESIGN GUIDELINES

### 3.3.1 Purpose and Intent

The following Design Guidelines have been developed as a method of achieving a high-quality, cohesive design fabric for the Kohl Ranch Specific Plan area. The objectives of these guidelines are:

- to provide the County of Riverside with the necessary assurance that the Specific Plan area will develop in accordance with the quality and character proposed herein;
- to serve as design criteria for developers, builders, engineers, architects and landscape architects, and other professionals in preparing plans for construction; and
- to lend guidance to staff, the Joint Powers Authority and the Board of Supervisors in the review and evaluation of future development projects in the Specific Plan area.

Certain key design elements will contribute significantly to the visual order and consistency of the entire Specific Plan area. These common features—site planning, architecture, landscape architecture, signage, lighting and other details—are the subject of the Design Guidelines. The guidelines express the desired character of future development, which is represented conceptually in **Figure 3.3-1, Community Character**. Each guideline will be considered in terms of how it relates to a given development proposal, during the review process, and will be applied to the selected land use concept, as appropriate.

The Design Guidelines are intended to be flexible and are illustrative in nature. Therefore, over time they can respond to unanticipated conditions, such as changes in taste, community desires and the marketplace. Fundamental principles that are the foundation of the Design Guidelines include the coordinated use of land for passive and active recreational open space, to handle storm drainage and to comply with restrictions of the Jacqueline Cochran Regional Airport Safety Zones. In addition, use of landscaping materials and architectural treatments should be compatible with the desert landscape as well as the rich agricultural tradition of the Coachella Valley.

Figure 3.3-1, Community Character

### Compliance with County Design Guidelines

The Design Guidelines and Development Standards for the Kohl Ranch Specific Plan are intended to establish community guidelines and standards for the project to ensure the creation of a quality and aesthetically pleasing environment. Section 3.4, Landscape Guidelines, contains the following subsections: Landscape Concept, Entries, Intersections, Streetscapes, Edges, Buffers, Landscape Palette, Paths and Trails, Walls and Fences, Lighting and Signage. Section C.4., Site Development Guidelines, contains the following subsections: Site Planning and Architectural Guidelines.

The Kohl Ranch Design Guidelines and Development Standards were developed prior to adoption of the Countywide Design Standards and Guidelines criteria (adopted January 13, 2004). Nonetheless, the adopted standards support the development of neighborhoods within the Specific Plan which will meet or exceed the intent and standards contained within the County-wide document. The Kohl Ranch Design Guidelines and Development Standards are consistent with the intent of the Design Strategies and Planning Objectives contained in the Countywide Design Standards and Guidelines (Sections I, Introduction, II, Residential, III, Residential Street Design, and IV, Landscape Design Standards). The Kohl Ranch Specific Plan complies with the standards, goals and objectives by, "*creating quality and visually distinctive development responsive to the natural and built environment of Riverside County*" (Countywide document, page 6).

Prior to approval of tentative tract maps, the Planning Commission and the Board of Supervisors may seek more site-specific information about whether or not the Countywide Design Standards and Guidelines will be met. Standards and features of design implementation illustrating specifically how these general guidelines will be implemented may help guide decision makers during public hearings. Therefore, a project proponent shall submit a design manual to the Planning Department for review and approval prior to public hearings on individual tentative tract maps. Each design manual shall be keyed to the Specific Plan planning area within the project site, and each shall identify how and where particular design features of note will be realized within the development.

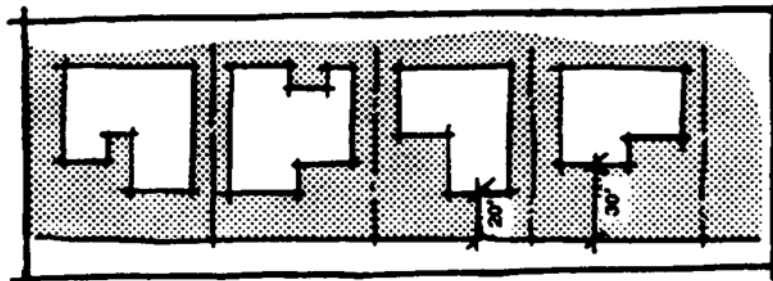
### 3.3.2 Site Development Guidelines

#### Site Planning

The intent of site planning is to integrate buildings and site improvements into a unified setting with minimal impact of the development on adjacent land uses. Site planning concentrates on the proper placement of buildings, roads and services.

The goal of residential site planning is to create a neighborhood fabric that offers a unique, safe and visually appealing environment to those in and around the development. The site plan should:

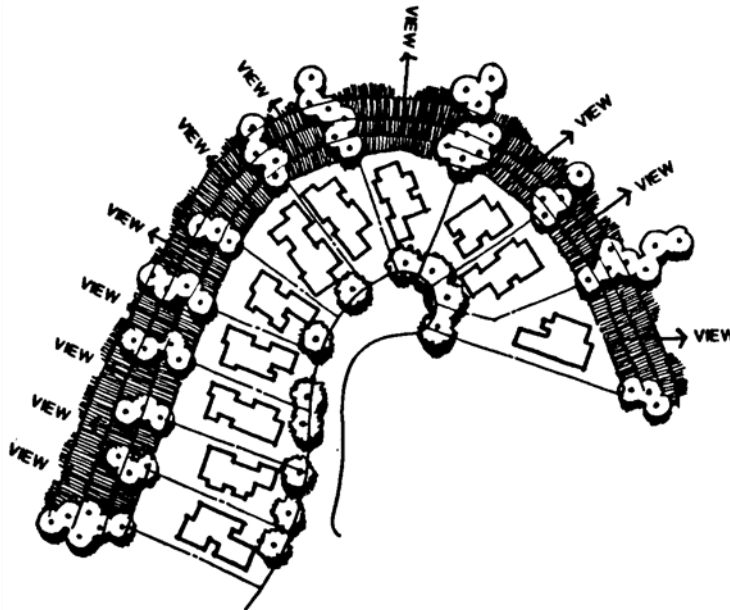
- Effectively accommodate the residential unit types proposed.
- Maximize unit exposure to parks, greenbelts and other amenities.
- Use of geometric "grid" layouts should be allowed.
- Cul-de-sac and curvilinear street layouts should be encouraged.
- Employ the latest techniques of energy-efficient/cost-effective subdivision design.
- Accommodate street drainage or underground drainage in accordance with generally accepted principals.
- Subdivision layouts should discourage through traffic while still permitting adequate emergency vehicle access.
- Variations of the building footprint with cut-outs and pop-outs can assure variety in a plan. Further variety and interest can be developed with plans by varying setback dimensions and positions of buildings.



*Varying Setback Dimensions*

### Single-Family Detached Residential Site Planning

- To the extent possible, units should have views from the rear of the units toward special features, including golf course and water features. Views should be enhanced with landscaping which does not block the view of adjacent lots.
- Single-family detached residential uses shall be buffered from existing adjacent land uses, and high traffic arterials. Landscape design should be used to provide privacy between units.
- A variety of building pad configurations can create further interest.
- Provide for variety along the streetscape.
- Orient views of the units towards special features, distant mountains, or common area open space. Views should be enhanced with tree plantings framing the desired view, but shall not block the view of adjacent lots.

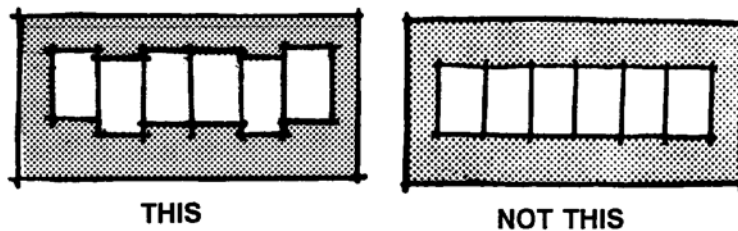


Views from Residential Units

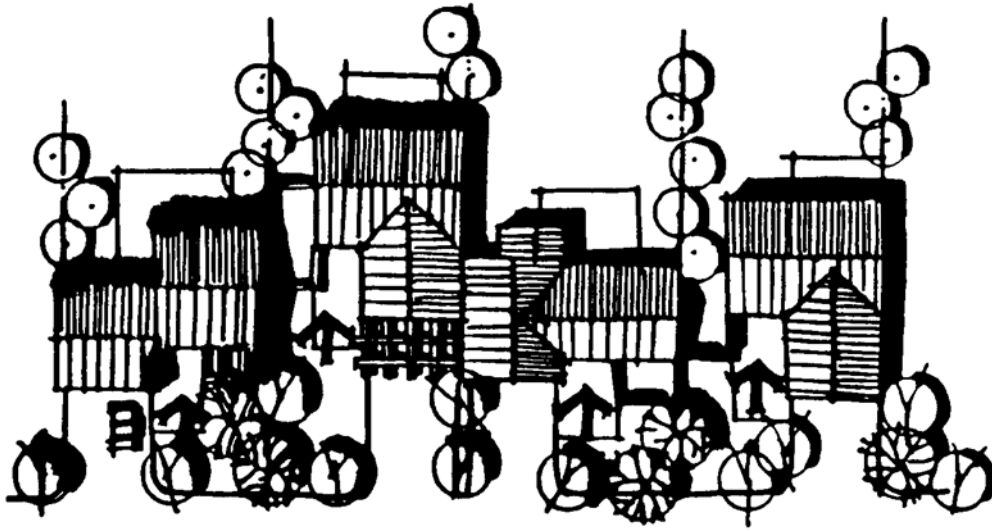


**Multi-Family Residential Site Planning****Multi-Family Housing Facades**

- Multi-family housing facades shall be detailed to give a varied quality to the building exterior rather than one long, continuous building. Attached buildings shall not be more than six dwelling units per floor.



- In higher density areas, where lot sizes permit, individual units shall be oriented in a variety of ways to avoid the monotony of garage door corridors.
- Maximization of open space is an important element within a higher density project. Curving the streets and orienting road axis to open areas and views can attain the feeling of open space. Open parking areas can be treated as landscaped plazas and courts.
- In multi-family and attached housing, it is important to provide each unit with its own entry and identity. This can be accomplished by offsetting and staggering each separate unit and by combining one and two-story building forms to separate massing. This also will provide variety to the streetscape.



*Streetscape Variety*

### Non-Residential Site Planning

Non-residential areas will serve the project site as well as the regional community. Businesses which locate here will rely on a high degree of visibility.



*Mixed Use/Air Park*

### Strong Linkages

- Links within commercial developments shall be reinforced by building and landscape elements. In the case of landscape elements, these links may be made with both hardscape (such as paving), or softscape.
- Although the area is to be auto-oriented with regard to attracting those shoppers from the local community, the site planning shall emphasize pedestrian, and bicycle links to minimize auto trips from within the Kohl Ranch. Interaction of residential, park and commercial areas is encouraged.
- In some instances objectionable elements may require visual screens and sound barriers. Otherwise, these treatments should be minimized and used only when necessary.



*Avenue 66 Commercial*

**Orientation of Structures**

- Buildings shall be arranged so that most store fronts are oriented toward and visible from the street and parking areas. The shape and configuration of the buildings will be determined by the site constraints. On corners or at major developments, satellite buildings shall be located closer to the street or at the corner, while the bulk of the building is placed far enough back from the street to allow circulation across the front of the parcels.
- Developments that are inward facing and that preclude through access are not encouraged, except when major attractions are destinations.
- Courtyards and outdoor seating areas are encouraged, and other ancillary structures, service areas, and trash enclosures shall be planned as part of the project, and placed or screened in a position of low prominence.
- 

**Commercial**

- Landscaping shall be used to break up otherwise uninterrupted building mass, frame views, and connect with development on adjacent pads.
- Building placement along a street should be varied to avoid creating a harsh, monotonous blockface or streetscape. Buildings should be sited so their entrances are generally oriented toward the street and parking areas.

**Office**

**Mixed Use Site Planning**

Mixed Use planning areas are established to encourage an innovative array of uses to include a combination of commercial, business, industrial, recreational, and residential uses.

- Mixed-use developments contain different building types. A variety of building types are permitted; commercial, business, industrial, race related facilities, residential, and duplex units.
- Where residential uses are mixed with nonresidential uses, the residential use should be located either above the nonresidential component or adjacent to the nonresidential component, as allowed by Section 2 – Zoning Ordinance.
- Proper building massing may be achieved through the use of sufficient vertical, horizontal and roof articulation of a building. Combinations of one and two story elements on the same building are encouraged to facilitate articulation.
- Consistent with the architectural style of the development, street facing facades should incorporate articulation and mix of color and materials to create diversity in the streetscape.
- Buildings within a development shall use colors, materials and architectural details that are compatible among buildings within the same development.

### Climatic Conditions

The effects of seasonal wind on development at the Kohl Ranch will, in some instances, be extensive. Wind may be intercepted, diverted or lessened. The following are suggested techniques of wind management:

#### **Shelter Belts on a Community Scale**

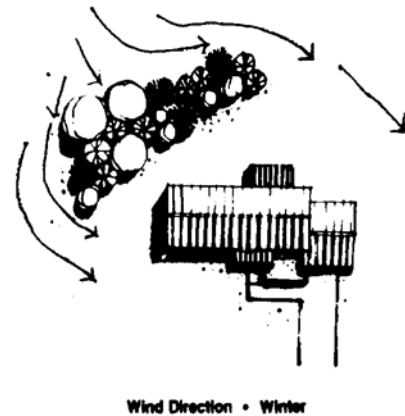
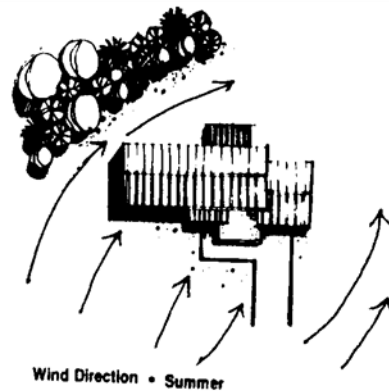
Shelter belts may be used in parallel rows to produce shelter on the leeward side. Belts with pinched profiles are less efficient in halting winds than belts with vertical edges.

#### **Solid Barriers**

Fences and walls are effective for protection but are very hard edges and should be limited in use. Barriers penetrated by the wind create more effective and wider wind shadows.

#### **Wind Breaks for Residential Use**

Wind breaks placed on residential property can route winter wind around the home and allow cooling breezes during the summer to penetrate the rear yard.



*Wind Breaks*

### 3.3.3 Architectural Guidelines

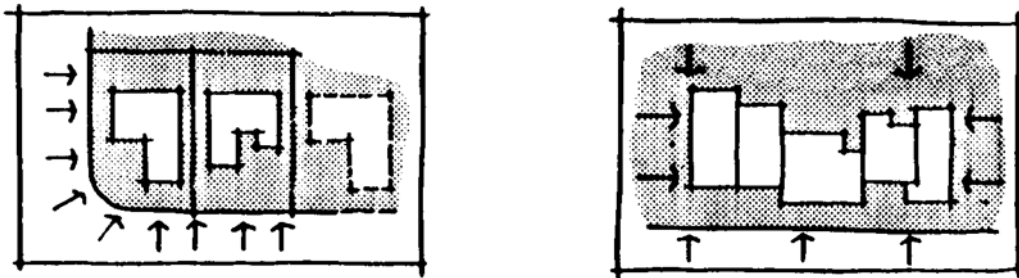
The architectural guidelines for the Kohl Ranch are intended to produce a feeling of authenticity without creating rigid, over-specific product/style requirements which often lead to architectural blandness and homogeneity. Rather, the Kohl Ranch should encourage a program of innovative design that reflects the historical desert landscape. The development programs should apply the guidelines in either literal or abstract forms.

These architectural guidelines are intended to establish design standards and form an encyclopedia of alternative design concepts.

#### Elevation Concepts

- These guidelines are primarily concerned with the physical appearance of the community environment as perceived by residents and visitors. Therefore, it is the intent of these guidelines to apply to the portion of a building that is visible to the public.
- Primary elevations are the surfaces of a building structure or yard that can be seen from public view (either vehicular or pedestrian). In a single-family project it usually means the front and/or exterior side yard views. In multi-family projects it may be all four elevations of a structure.

#### Primary Elevations



- Each building shall relate in terms of bulk and mass but should not be identical. A two-story building is more harmonious to a neighboring one-story building if it contains a one-story element.

#### Harmony of Elevations



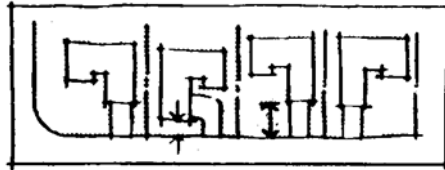
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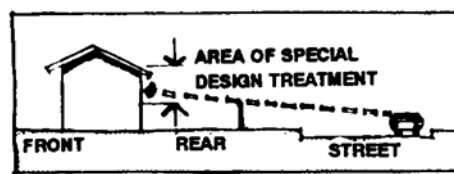
- Avoid stereotyped units which will produce monotonous elevations and street scene.
- A variety of elevations utilizing differing setbacks such as swing-in garages and reverse plans will ensure a varied street scene.

#### Variety of Elevations



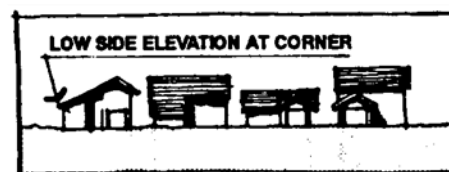
- Where applicable, the rear side of the building located adjacent to streets and other areas of high visibility should have similar treatment as with the primary elevation criteria. If a fence is used as a screening device, then it too should be designed to be part of the architecture.

#### Special Treatment Area



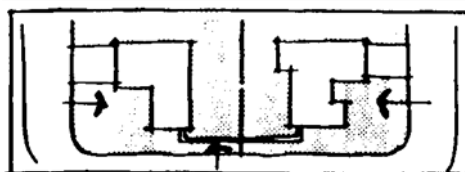
- Corner Elevations - Place the short side elevation on the corner to reduce the feeling of height and mass at the corner. Auto entry on a corner lot is also an important consideration. The high side can be placed on the corner as long as the side elevation is treated as a primary elevation.

#### Corner Elevations



- Elevations and Side Streets - Fences or walls that connect two separate units should be of the same material and color and be compatible with the architecture of the buildings.

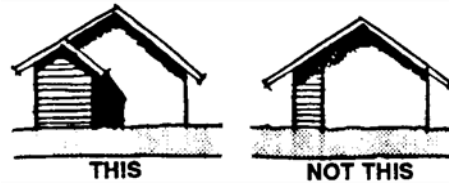
#### Elevations and Side Streets





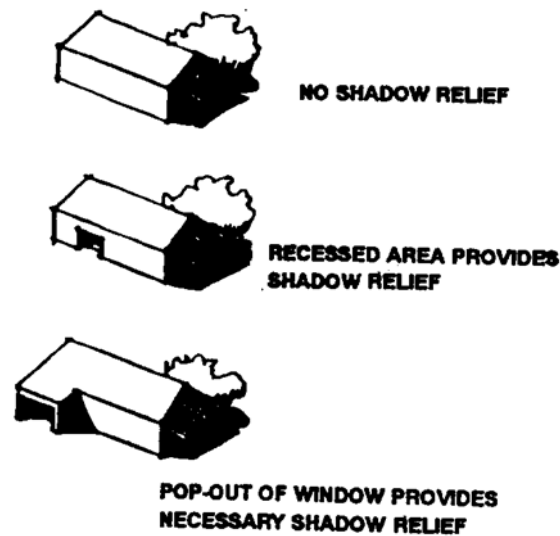
- Facades - If material changes do not occur in the same plane and if they intercept an architectural element, they are more effective.

**Facades**



- Allow for visual relief for interest and to break up long planar surfaces. Offsets, pop-outs, overhangs, and recesses all may be used to produce effective shadow interest areas. Larger buildings require more relief than do smaller buildings.

**Shadow Interest Areas**

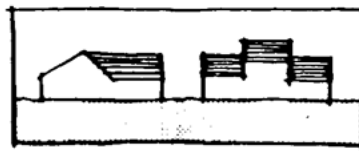


- It is desirable to accompany a plan offset with a change in the roof pitch orientation. This is important on the primary elevations.

**Roof Pitch**



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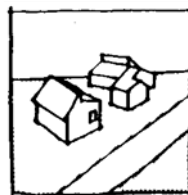
- The design theme should be continued throughout the project, including accessory items such as lighting fixtures and address plaques.

**Design Theme**

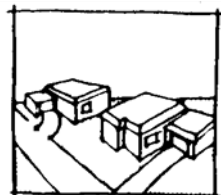


- Roof Design - Flat roof design for residential structures should be discouraged. If used, flat roofs should be a secondary design element only and should be contained within the scheme of an individual building design. Particular consideration as to color and material should be given to the design and treatment of roofs because of their visual impact.

**Roof Design**



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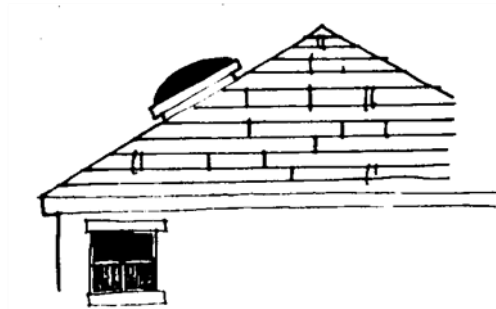
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## Details

There are many building elements that come into view. Usually they are secondary adjuncts to major element, such as a skylight on a roof. In some cases they become no more than a texture, such as a trellis. It is important in all cases, however, that these elements integrate with the whole. What may start out as a minor feature can become a distracting eyesore, if not thought through.

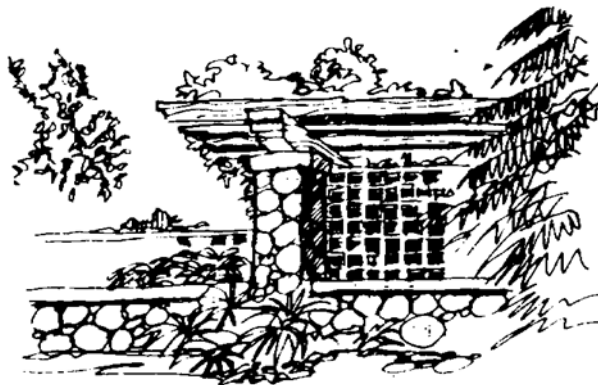
- Skylights should be integrated into the roof form.

### Skylights



- Trellis/Pergolas can be very pleasing and functional structures, (i.e., carport, entry canopy). They provide partial shade, screens for privacy or an arbor for climbing plants. They offer the warmth and texture of wood at a relatively low cost. It is important to consider a trellis as a permanent structure and design it accordingly. Avoid corrugated fiberglass metal, or other temporary type material. Use adequately heavy members for the support structure working down to no less than 2" nominal for the smallest dimension of the lighter members. The trellis will look more substantial and will not bow from old age prematurely.

### Trellis/Pergola



- Chimneys are usually very strong roof elements. Codes require that they extend higher than adjacent roof lines. Careful choice of proportion and material should give them a substantial and stable appearance.

- The use of prefab fireplaces and flues is economically sound. However, they need not have the stove-pipe appearance, depending of which spark arrester is used or required by fire code. Enclosing the flue in a masonry or wood chimney with substantial proportions will solve the problem.

### Materials and Finish

In keeping with the goals of the Kohl Ranch to maintain and preserve the natural surroundings, designs in harmony with nature should be extended to material, finish and color selection.

- Natural materials by their nature inherently work well with the surroundings. Natural colors or transparent finishes on these materials not only enhance them, but actually improve with age and are low maintenance. Man-made materials of natural colors and textures are also suitable.
- Brick, natural tile, concrete, steel and glass, if used honestly and in a straight-forward manner, can be pleasant. Large expanses of a single material, especially if unbroken by detail or depth, can become overpowering to the rest of the building form and the surroundings. Conversely, over-detailing with use of too many different materials or textures can create confusion and distract from an otherwise good design.

### 3.3.4 Lighting

Lighting should be designed to ensure compatibility with the community architectural and street furniture themes, and the safety of the site users. An effort should be made to emphasize "human scale" in public areas adjacent to buildings and along walks. Streetlight standards, traffic signal poles, and pedestrian and building lighting play a vital role in defining a safe and secure appearance. This lighting concept has been devised to provide a hierarchy of lighting effects which will contribute to the overall cohesiveness of the community image.

#### General Guidelines

- Warm white lighting is encouraged. Bright colored or blinking lights are not encouraged except in theme restaurants and shops of commercial development areas.
- Building or roof outline tube lighting shall be subject to County of Riverside approval.
  
- Design and placement of site lighting should minimize glare affecting adjacent properties, buildings, and roadways.
- Careful consideration and coordination shall be given to avoid any potential conflicts with Jacqueline Cochran Regional Airport operations.
- Lighting shall be designed to minimize sky glow and effects on the Mt. Palomar Observatory and the nighttime desert sky.
- Fixtures and standards shall conform to state and local safety and illumination requirements. In particular, lighting shall conform to Riverside County Ordinance No. 655, which includes requirements related to the Mt. Palomar Observatory.
- Automatic timers on lighting should be designed to maximize personal safety during nighttime use while saving energy.

#### Roadway Lighting

Lighting fixtures and standards within the public right-of-way play a crucial role in displaying a level of quality for the development.

- Lighting shall be positioned to enhance the safety of vehicular and pedestrian flows at key points along the roadway. Light shall be concentrated at intersections and pedestrian crosswalks. Intersections may have increased wattage for definition and to alleviate automobile/pedestrian conflicts.
- The maximum height of roadway lighting should be twenty-five (25) feet with a minimum clearance of sixteen (16) feet above roadways.

#### Parking Area Lighting

Since landscaped islands within parking areas are adjacent to high use commercial, office, and recreation buildings, they typically may be hidden from view by landscaping and trees, therefore requiring a less overt lighting standard. Conversely, they may be highly refined and emphasized as a design element within commercial areas where visibility to buildings is crucial.

- Stone or concrete, limited to twenty-four (24) inches in height, may be used for light standard bases in parking area islands to protect lighting from automobile damage.

### **Pedestrian and Entry Area Lighting**

Pedestrian and entry area lighting should be provided at plaza areas, improved trails, entryways, courtyards, parking lots and other public spaces requiring night illumination.

- For all uses permitted within the Kohl Ranch, where above ground illumination is desired, a lighting fixture and standard compatible with the community theme is recommended.
- In pedestrian areas the use of low level walkway lighting is preferred over lighting from above. Lights shall be positioned no higher than four feet above grade.
- Concrete or metal light bollards not exceeding four feet in height may be used to accentuate building entrance areas or to provide separation for pedestrians from vehicular traffic lanes.
- For pedestrian walkways, point-to-point lighting is acceptable with no specific illumination levels required. The main emphasis shall be to clearly identify the pedestrian walkway and direction of travel. These lamp elements may become an important design element on the landscape, or they may be sublime and unobtrusive.
- Fixtures which shield and limit spillover light into the night sky are recommended.

### **Architectural Lighting**

- To highlight monument signs or architectural features such as walls, entry ways or lobbies, dramatic lighting should be used. Spillover lighting from lobby areas is acceptable although glare onto adjacent properties should not occur. Service area lighting shall be contained within service yard boundaries, with light sources concealed.
- Building illumination and architectural lighting shall be creative and must reinforce the design theme. Indirect wall lighting or "wall washing", overhead down lighting, or interior illumination which spills outside is encouraged. Wall "washing" effects shall be sparingly used to accentuate architecture at key visual areas and not an attempt to advertise buildings to roadways.
- Lighting shall not cast any glare onto adjacent lots and streets in such a manner as to decrease the safety of pedestrian and vehicular movement.
- Architectural lighting shall be used to articulate the particular building design. Rim lighting of eaves, bending, uplighting, wall washing, and other effects shall be used in compliance with the design theme for the planning area. Architectural lighting shall be integrated with building elements and concealed flush with grade wherever possible.
- Building or wall lighting shall be indirect. A limited number of lights may be used to create shadow, relief and outline effects.
- Lighting shall be used to enhance landscaping and reinforce architecture, with dramatic uplighting or wall shadow effects with plant materials encouraged.
- Parking lot light fixtures and bollards shall be consistent in styling with the design theme proposed for that subarea.
- Light standards shall not exceed 20 feet in height or the height of the building, whichever is less.
- All architectural lighting should be carefully integrated into design themes and should not be visually obtrusive and incongruent during the daytime.

### **Landscape Lighting**

- Landscape lighting can be used to highlight landscape features such as specimen trees and pedestrian areas. As with architectural lighting, light sources should be designed as an integral design element, concealed flush with grade, or hidden by vegetation during the daytime.
- String lights, with small nonflashing bulbs, may be used to highlight trees and similar features within the golf course clubhouse, commercial areas in interior courts only, and other similar outdoor areas at night to create excitement and a festive ambiance.

### **Athletic Field, Court and Driving Range Lighting**

For proper lighting of athletic fields, driving ranges and courts in recreation facilities of parks and schools, spillover lighting may occur. Lighting shall be designed to minimize the spillover effect. These areas shall be carefully site planned to impact as few neighbors as possible. Hours of operation shall be established which restrict intrusion of nighttime lighting, and posted at visible locations at these recreations areas.

### 3.3.5 Signage

#### General Guidelines

- All signs within the project should be designed to provide a consistent reinforcement of the Kohl Ranch character. Signs must communicate not only specific information but should, in themselves, add to the attractiveness of the area.
- Signs in the project area should advertise a place of business, a residential area or provide directions and information.
- Signs should not compete with each other or dominate the setting via inconsistent height, size, shape, number, color, lighting or movement.
- Signs shall conform to the guidelines set forth herein and with Riverside County Ordinances.
- No signs shall be placed upon or project into or above public property or the public right-of-way, except as noted herein.
- Signage typeface should be consistent with building architectural style.
- Public signs should be limited to those absolutely necessary for smooth traffic flow, safety, and public information.
- Clear sight triangles should be observed and enforced at all intersections, corners, parking areas, and drives at all times.
- All signage should be maintained in good repair, including the display surface, which shall be kept neatly painted or finished.
- The exposed backs of all signs visible to the public should be suitably finished and maintained.
- All signage should be designed free of bracing, angle-iron, guy wires, cables, or similar devices, except for commercial advertising signs as hereafter noted.
- Painted signs should use fade-resistant, durable, exterior finish paint. No luminous, fluorescent or iridescent paints or plastics are permitted. Permanent wooden signs or materials that may be degraded by the climate should be avoided unless adequately sheltered.
- An effort shall be made to achieve consistency between building style and sign design. In all cases, signage should be complimentary to the exterior treatment of the building or location involved.
- Color schemes for signage shall relate to other signs, graphics and color schemes in the vicinity in order to achieve an overall sense of identity.
- Signs may be lighted; however, no light that flashes or blinks or affects changes in hue or intensity of illumination is permitted. Illumination sources for any sign shall be hidden from view.

#### Community Monuments

The following sign standards will effectively regulate the placement, erection and maintenance of permanent signage within the Kohl Ranch. These standards are intended to provide equitable standards for the protection of property values, visual quality and public health, safety and general welfare. The hierarchy of signs for the Kohl Ranch is as follows:

#### Project Entries

- Monument signs shall identify the Kohl Ranch community at key project entry points. These monuments should be the most prominent in scale and should set the overall theme for the entire community.
- The wall or monument materials should establish the project theme which other developments should use as a general guide.



- Project entry signs should be designed with the angle, distance and length of view in mind. Signage viewed from vehicles should be large and/or conspicuous enough to be read quickly. Signs designed to be viewed from slower speeds or at shorter distance may have more attention given to details such as texture, and may have a reduced text size.
- Building materials should consist of native stone, river rock cobblestone, concrete, brick, stuccoed concrete block, wood rails and boards, metal lettering systems and logos, sheet metals and wires, and special architectural elements such as weather vanes or windmills.
- Maximum dimensions for message areas of signs should be limited to those established in the County of Riverside Ordinance No. 348.
- Lighting should be limited to those fixtures which are at grade or are concealed by landscape planting.
- Signage typography should be limited to bronze, copper, or any quality steel logos or channel letters.

#### **Residential Neighborhood Entries**

- Residential neighborhood entries should establish the theme of the individual development while continuing the community theme.
- Recommended building materials may consist of native stone, river rock cobblestone, concrete, brick, stuccoed concrete block, wood rails and boards, and special architectural theme elements.
- Residential entry monuments should be less grand in scale than the major intersection treatments. The monuments should be in proportion with the surrounding streetscape and should be highlighted by landscape plant materials and site furnishings. The monument may be part of a wall, or stand alone.
- Maximum dimensions for message sign areas shall be limited to dimensions established by Riverside County Ordinance No. 348.
- Graphic signage materials should be limited to bronze, copper or any other high quality steel logos or channel letters.
- Back-lit letters or a concealed lighting source, located flush with grade or within a covered entry structure may be used.

#### **Commercial Entries**

- Commercial entries should incorporate the commercial center building materials, while continuing the community theme. Entry monuments should be subdued and not over-powering. They should simply and discreetly identify the development rather than create a grand entry statement.
- The maximum dimensions of the sign base and message area shall adhere to Riverside County Ordinance No. 348.
- Signage typography should be limited to bronze, copper or quality finished steel logos or channel letters. Sign text should be limited to identification of the street address, project name, and major retailers.
- Back-lit letters or a concealed lighting source, located flush with grade may be used.
- Signs may be constructed on brick, native stone, wood or metal. Letters may be mounted upon the signage base.
- Monuments shall be integrated with berming and landscape materials with established streetscape design themes.

### Retail Signs

- Only one monument sign shall be allowed per building. These signs shall be located at entry points from parking areas, adjacent to walkways, in planting beds or within building setback landscape areas adjacent to the street right-of-way.
- Signs constructed of brick and/or native stone base, a smooth finished concrete, carved wood or metal panel systems are recommended.
- Sign dimensions shall adhere to those established by County Ordinance No. 348. Letters should be consistent with the architectural material of the establishment.
- A street address is recommended to be incorporated into the sign banner. Lighting should be concealed by plants or flush with grade.

### Directional Signs

Directional signage, used to identify and direct vehicular and pedestrian traffic to on-site destinations shall be provided along roadways and within all multi-parcel and multi-tenant developments. Directional signage should be of consistent design throughout each project. All such signage should be fabricated from the same materials, with a consistent color palette and common graphic theme. The use of materials compatible with the architectural design of each project and its corresponding site furnishings is encouraged. Directional signs should be of consistent dimensions, and located in a visually logical order.

These signs should provide on-site directional information and should not be used for advertising. They should include the following:

- Directory monuments
- Public service signs
- Traffic safety signs
- Residential street signs

#### Directory Monuments

- Directory monuments should be permitted as ground signs or on the faces of buildings or structures to illustrate the project layout and locate the establishments or residential buildings of the development.
- The sign area of such location markers should be designed for either pedestrian or automobile users.
- The location and number of these markers should be determined at the individual project design level.
- All dimensions, material types and signage area requirements shall conform to County Ordinance No. 348.

#### Public Service Signs

Service signs should only provide general public information to direct the way to public facilities such as information centers, rest rooms, telephones, emergency stations, etc. Service signs should be permitted as wall or ground signs and should be limited in number and location as outlined below.

- Signs are permitted on the faces of buildings or structures provided that such signs should be placed at eye level above the immediately adjacent ground.
- Signs setting forth the location of, or directions to, parking or buildings located on the premises, or regulating the flow of on-premise traffic, should be permitted as part of the separate free-standing ground structure or kiosk. Such signs may include pictorial and decorative designs.

- The signs should include a minimum of words and numbers necessary to accurately communicate the required information.
- The signs shall be compatible with the adjacent architectural theme.

**Traffic Safety**

Traffic safety signs shall be subject to the standards of the County and State agencies.

**Residential Street Signs**

All street signs shall be consistent with the overall Kohl Ranch theme.

**Temporary Signs**

Signage that identifies uses or activities temporary in nature, such as that associated with real estate sales and leasing or the development and construction of buildings, shall be allowed on a temporary basis if consistent with the design character of the surrounding area. No illumination of temporary signs should be permitted. Temporary signage should be located on or in close proximity to the uses identified. The multitude of signs associated with development, design, construction and leasing should be combined into one sign for each project and the sign should be located on site.

**Project Marketing**

- These signs should provide community recognition for the Kohl Ranch during the construction and sales period.
- Wood with a painted sign surface is the recommended construction material. Signs may be double-sided. Signs may be painted onto the temporary construction safety walls erected to hide construction from view.

**Non-Residential Construction and Leasing**

- These signs are intended to inform the viewer of new buildings and leasing opportunities, opening dates, and builder names and telephone numbers of individual parcels within the Kohl Ranch project.
- Wood construction with a painted sign surface is recommended for this application.
- One sign per individual parcel or project should be allowed and should be located on-site.
- Sign face may include any sketch or architectural rendering of the proposed use.
- Sign removal should occur after lease out.

**Residential Sales and Leasing**

- Temporary ground signs for real estate sales, leasing, construction or model homes are permitted. One project identification monument or ground sign should be permitted for each major entrance to a development. Sign copy is limited to project name and address in addition to the word "sales" or "leasing."

**Prohibited Signs**

- Obsolete or Abandoned Signs. Any sign located on vacant or unoccupied property that was erected for a business which no longer exists, or any sign which pertains to a time, event or purpose which no longer exists, shall be removed within 30 days after the use has been abandoned.
- Signs constituting a potential traffic hazard or which simulates or imitates in size, color, lettering or design any traffic sign or signal.

- Temporary signs on public property (street, median island, parkway, sidewalks, traffic control sign posts, utility poles, park land, trees, etc.)
- Animated or moving signs: signs consisting of any moving, swinging, rotating, flashing, blinking or otherwise animated components.
- Off-premise signs: any sign, other than a simple directional sign, installed for the purpose of advertising a project, development, event, person or subject not related to the premises upon which sign is located.
- Vehicle signs: signs on or affixed to trucks, vans, automobiles, trailers or other vehicles which identify, or provide direction to a use or activity not related to its lawful making of deliveries or sales of merchandise or rendering of service from such vehicles.
- Portable signs: a freestanding sign not permanently affixed, anchored, or secured to the ground or structure it occupies, including tailored signs, except as approved by the County of Riverside.
- Roof signs: any sign erected, constructed and maintained upon or over the roof of any building, unless it is a projecting canopy sign or is an explicit part of the building architecture.
- Other prohibited signs: advertising signs and billboards, inflatable signs or balloons, inflatable animals, magnetic signs, plastic flags unless otherwise specified in these guidelines or approved by the County of Riverside.

### 3.3.6 Walls and Fences

Walls and fences are of particular functional importance for the Kohl Ranch as depicted in **Figure 3.3-2, Wall and Fence Plan**. Other allowed uses in open space that are in the vicinity of the airport safety zones will also be appropriately fenced or walled. If the golf course or motor sports race track are implemented, an individual fencing plan for those uses will be prepared.

Walls and fences establish enclosure, confer physical and visual privacy and also provide vertical texture. Walls and fences are especially important in creating a theme for the residential portion of the community. They should, however, be used sparingly and with great consideration so as not to detract from the open space or to simply demark property lines. The material, style and height of walls and fences shall provide an element of continuity throughout the project to insure visual consistency. The walls and fences should be designed with the intent of furthering the architectural character of the site.

Walls may be solid, perforated, or hidden from view when security is required, but view retention is desired. As fences are subject to severe exposure they must be well-made. All community and perimeter project walls and fencing are to be provided by the project builder at the time of development. Wherever practical, plant material should be used as a barrier rather than walls and fences.

#### General Guidelines

The horizontal mass of continuous walls should be softened by landscaping as specified in the landscape guidelines. No wall or fence shall exceed 6 feet in height unless it is used for noise attenuation where a combination of wall and berming is not sufficient. If retaining walls are necessary they should be terraced and should not exceed this 6-foot height limit.

Long stretches of unrelieved walls and fences should be broken up with varied setbacks or recesses for plantings. When a change in pad elevation occurs, the wall or fence should be stepped in equal vertical intervals. No step should exceed 12 inches in height.

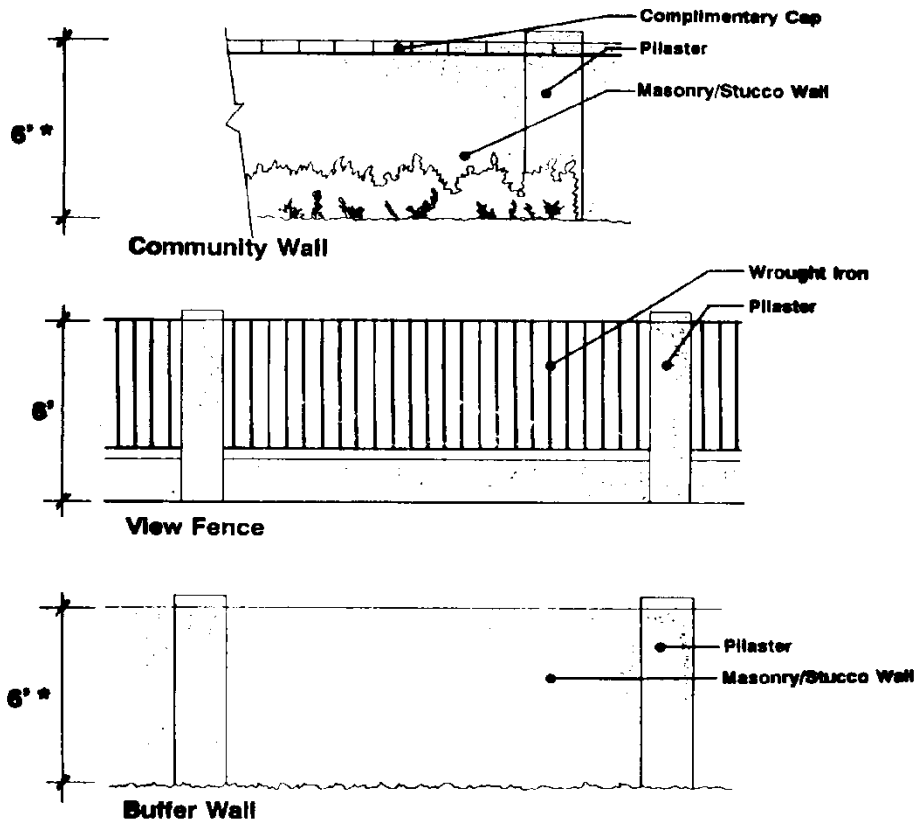
#### Community Walls and Fences

Community walls and fences will provide community identity, security, privacy and sound buffering for residential units adjacent to project streets or incompatible land uses as depicted in **Figure 3.3-3, Wall and Fence Types**.

- Solid theme walls may be required along major thoroughfares where sound attenuation and privacy are required and when residential areas are adjacent to schools, commercial areas, or parks.
- Solid walls along residential development edges visible from surrounding arterial roadways shall be composed of solid masonry covered with smooth stucco. The wall will stand six (6) feet in height. Walls should be planted with vines or screened with other plant materials to reduce their visual impact. This will contribute to the shade and residential environment of the development the wall is intended to screen.
- Wall materials shall consist of simple masonry construction finished with colored smooth stucco, consistent with the desert color palette.

**Figure 3.3-2, Wall and Fence Plan**

Figure 3.3-3, Wall and Fence Type



\*Unless otherwise determined by a noise study for an adjacent land use.

**Perimeter Walls and Fences**

Fencing should be allowed within the project area and along the project boundaries. Landscape hedges and other screening measures should be used to reduce the visual incongruity of such fences.

**View Fences**

These fences should be used to increase the sense of openness in rear yards, along open space, golf courses, parks and schools.

- If golf courses are built, fences adjacent to golf courses visible from surrounding arterials should be view fences. Recommended material includes wrought iron, wood pickets, or welded wire on wood posts. Spires, spikes or other detailing consistent with the community theme may be used to impede trespassing over metal rails.

**Privacy Walls and Fences**

The privacy walls and fences are utilized alongside and rear lot lines where residential units need more privacy. Recommended material includes wood panels, masonry, stucco and stone.

**Theme Walls and Fences**

In certain instances such as project entries and at restaurants, thematic fences or walls may be appropriate. An example would be a western steak house with a low profile split rail fence at the entry. Special conditions that warrant thematic uses shall be approved by the County of Riverside.



## 3.4 LANDSCAPE DESIGN GUIDELINES

### 3.4.1 Landscape Concept

The landscape guidelines for the Kohl Ranch shall contribute greatly to the establishment of a community theme for the project. The landscape concept shall transcend all individual parcel boundaries and unify the community "edges," those areas along roadways, parks, commercial shopping areas, undisturbed areas and drainage ways and the adjacent residential development areas.

The landscape concept provides criteria for the treatment of all areas within roads and easements, medians, development edges, and project entries. The concept has been conceived to organize and present a memorable image for the Kohl Ranch project. Its purpose is to establish a unified landscape framework that provides continuity throughout the project area, and encourages the desert community theme. To accomplish this purpose, these guidelines should be consistently applied to define major project roadways (expressways, arterials, secondary and major highways, collectors, industrial collectors, and loop roads) roadway medians, internal circulation routes, major entries, and intersections as reflected in **Figure 3.4-1, Landscape Key Map**. Landscape materials within these areas should direct and guide the resident or visitor to the development, screen sensitive views, and frame or create focal points and views as the motorist, pedestrian, or bicycle moves through and around the project. A high degree of landscape quality is to be maintained throughout the Kohl Ranch Specific Plan area. Key elements of the concept are presented in **Figure 3.4-2, Community Structure Plan**.

The following are key objectives that the landscape concept should foster:

- The Kohl Ranch project should create a landscape theme that is compatible with the native desert community and architectural and site planning themes.
- The landscape is intended to work in association with the architectural guidelines and the grading plan to create screens and buffers where needed and views where the opportunity exists.
- The landscape should serve to augment sound attenuation efforts made with the landform or hardscape elements.
- Plant material selection for streetscapes, entry areas and development areas should be limited to those plants listed in the suggested plant palette.
- Landscape plant materials should be "long lived" varieties. "Short lived" materials such as flowering annuals and perennials may be utilized to accent or augment the "longer lived" base elements which form the landscape framework.
- Landscape elements within the front building setback visible from the public rights-of-way should blend with street edge landscaping.

**Figure 3.4-1, Landscape Key Map**

**Figure 3.4-2, Community Structure**

- All common landscaped areas shall be designed with the objective of reducing long-term water use to a minimum in accordance with the Coachella Valley Water District Ordinance 1302.1 Landscape and Irrigation System Design Criteria and the Kohl Ranch Plant Palette. Native and drought tolerant plant materials and water-efficient irrigation practices shall be used. Within the golf course(s), if developed, water-efficient landscaping shall be encouraged in the "rough" and peripheral areas.
- Recycling of irrigation water is encouraged.

It is the intent of the landscape guidelines to foster innovative design and discourage the bland and mediocre. The design of the Kohl Ranch draws upon the character of the natural setting and applies this theme to concepts which are both cost effective and maintainable. Furthermore, the landscape guidelines foster a spirit of stewardship for the project open spaces and create a symbiotic existence with the developed areas, ultimately creating a unique recreational amenity for the community.

The essence of the landscape guidelines consists of the elements described below.

### **Community Identity**

These guidelines establish a benchmark for high quality community landscape architecture by establishing guidelines for essential landscape elements that carry forth the landscape concept rather than relying upon a static and all inclusive plant palette. This flexible concept will allow for personal expression for individual builders and their designers within the community while maintaining a community structure and theme.

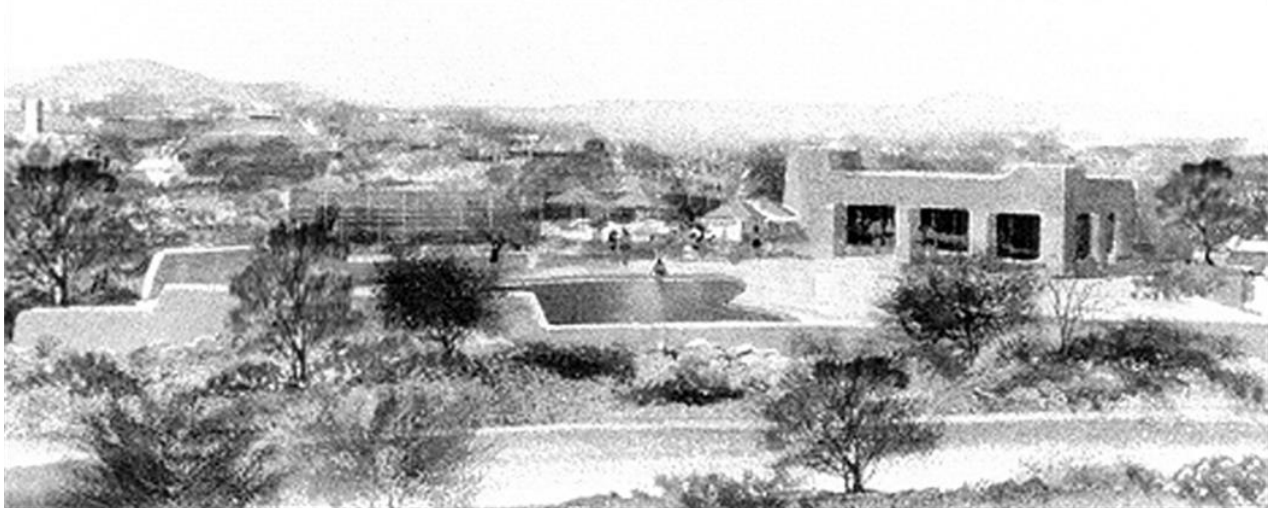
### **Visual Screens**

The guidelines form a program which mitigates the effects of the built environment upon the undisturbed open space. The landscape guidelines also provide concepts for buffering buildings from adjacent land uses, both on and off site.

### **Conservation of Resources**

The landscape plan and suggested plant materials have been selected with careful regard for the Coachella Valley's water supply and CVWD requirements and therefore, the resulting long-term viability of the landscape. The landscape will be efficient in terms of water consumption as well as maintenance requirements. Resources shall be concentrated in those areas of most intense human use, such as parks and other high-use areas. All common landscaped areas shall be designed with the objective of reducing long-term water use to a minimum. The use of native and drought tolerant plant materials and the use of water-efficient irrigation practices and the development of programs which "wean" plant materials from irrigation after their establishment are encouraged, while the use of high water-consuming plant materials shall be limited to selected areas. The landscape concept should encourage minimizing long-term maintenance for the majority of landscaped areas. This will be achieved by limiting areas of turf, clipped hedges, and exotic ornamentals to special places where they can be emphasized. Drainage channels through the site will be developed with native desert landscaping.

Alternative sources of irrigation water shall be considered, including reclaimed water, Colorado River water and surface runoff. The proximity of the site to Water Reclamation Plant No. 4 would allow for the use of reclaimed water for irrigation once tertiary water becomes available.



**Neighborhood Park**

The landscape guidelines encourage the use of the landscape as more than a visual aesthetic by creating environments that use plant materials as architectural elements which define space, establish proportion, and influence climate. The guidelines will encourage spaces which will, through the thoughtful arrangement of plant materials, encourage a range of emotions from intimate enclosure to vast openness. The landscape should affect the tactile senses by creating microclimates where people can escape the heat of summer or be warmed by the sun in the winter. The landscape should provide pleasing odors of flowers, rustling sounds of leaves in the wind, in addition to visual screens and focal points.

#### **Application of Concepts**

The guidelines provide a detailed, user-friendly landscape plan and plant matrix which outlines specific treatments and suggested plant materials to carry out the community theme. This plan and matrix should be easily understood by individual developers, builders, and designers.



The Community Structure Plan (Figure 3.4-2) shows how the various landscape conditions interact with one another to create the overall community landscape theme. In general, the plan is concerned with those areas within road rights-of-way, drainage areas, parks, and visually sensitive development areas such as entries and focal points.



### 3.4.2 Landscape Plant Palette





The following plant palette has been derived to enhance the Kohl Ranch desert landscape theme. The plant material has been selected to accent the desert environment while providing important functional characteristics. This list is intended to be a menu from which landscape architects can mix and match plant materials for various situations. The following general criteria have been established to determine plant selections:

- Desert Compatible Plants – Plants shall be tolerant of the harsh desert environment.
- Low/Moderate Water Consumption – Plants should not be high water consumers. Vast expanses of lawn areas are discouraged. Golf courses shall be primary water consumers and shall be irrigated with canal water to the extent possible.
- Broadleaf Deciduous Trees – These plants should be used where the need for summer shade and winter sunshine is important.
- Groundcover – Use of drought tolerant groundcovers and decomposed granite should be encouraged to reduce blowing dust.
- Maintenance of Established Themes – The Kohl Ranch streetscape and entry themes should be maintained (i.e., Palm trees evenly spaced with citrus understory and desert groundcover throughout).


**Table 3.4-A, Kohl Ranch Plant Palette**




Plant Form	Botanical Name	Common Name	Location
<b>PALMS</b>			
	<ul style="list-style-type: none"> <li>• <i>Arecastrum romanzoffianum</i></li> <li>• <i>Brahea armata</i></li> <li>• <i>Chamaerops humilis</i></li> <li>• <i>Phoenix canariensis</i></li> <li>• <i>Phoenix dactylifera</i></li> <li>• <i>Washingtonia filifera</i></li> <li>• <i>Washingtonia robusta</i></li> </ul>	<p>Queen palm</p> <p>Mexican blue palm</p> <p>Mediterranean fan palm</p> <p>Canary Island date palm</p> <p>Date palm</p> <p>California fan palm</p> <p>Mexican fan palm</p>	<p>Palm Tree Streetscape for Major and Minor entries, Intersection Details, Expressway Streetscape, Urban Arterial Streetscape, Arterial Streetscape 1 and Major Streetscape 1.</p>
<b>LARGE SHADE TREES</b>			
	<ul style="list-style-type: none"> <li>• <i>Brachychiton populneus</i></li> <li>• <i>Cassia leptophylla</i></li> <li>• <i>Dalbergia sissoo</i></li> <li>• <i>Fraxinus velutina</i></li> <li>• <i>Koelreuteria bipinata</i></li> </ul>	<p>Bottle tree</p> <p>Golden Medallion</p> <p>Rosewood</p> <p>Arizona ash</p> <p>Chinese Flame Tree</p>	<p>Formal Canopy Streetscape for Expressway Streetscape, Major Streetscape 2, Secondary Streetscape 1, Secondary Streetscape 2, and Collector Streetscape 1.</p>

Plant Form	Botanical Name	Common Name	Location
	<ul style="list-style-type: none"> <li>• <i>Olea europaea</i></li> <li>• <i>Pinus Eldarica</i></li> <li>• <i>Platanus wrightii</i></li> <li>• <i>Quercus virginiana</i></li> <li>• <i>Tipuana tipu</i></li> <li>• <i>Ulmus parvifolia</i></li> </ul>	<p>'Swan Hill' olive Afgan Pine Arizona sycamore Southern live oak Tipu Tree Evergreen Elm</p>	
	<ul style="list-style-type: none"> <li>• <i>Acacia salicina</i></li> <li>• <i>Acacia farnesiana</i></li> <li>• <i>Acacia smallii</i></li> <li>• <i>Acacia stenophylla</i></li> <li>• <i>Cercidium floridum</i></li> <li>• <i>Cercidium h. 'Desert Museum'</i></li> <li>• <i>Chitalpa Tashkentensis</i></li> <li>• <i>Geijera parvifolia</i></li> <li>• <i>Parkinsonia aculeata</i></li> <li>• <i>Prosopis alba</i></li> <li>• <i>Prosopis chilensis</i></li> <li>• <i>Prosopis hybrid 'Phoenix'</i></li> <li>• <i>Rhus lancea</i></li> </ul>	<p>Willow acacia Sweet acacia Sweet acacia Shoestring acacia Blue palo verde Hybrid palo verde Pink Dawn Australian willow Mexican palo verde Argentine mesquite Chilean mesquite Hybrid mesquite African sumac</p>	<p>Formal Canopy Streetscape for Expressway Streetscape, Major Streetscape 2, Secondary Streetscape 1, Secondary Streetscape 2, and Collector Streetscape 1.</p>
<b>STREET TREES (NON-DESERT)</b>			
	<ul style="list-style-type: none"> <li>• <i>Brachychiton populneus</i></li> <li>• <i>Eucalyptus species</i></li> <li>• <i>Fraxinus velutina</i></li> <li>• <i>Platanus wrightii</i></li> </ul>	<p>Bottle tree Various Arizona ash Arizona sycamore</p>	<p>Informal Canopy Streetscape for Urban Arterial Streetscape, Arterial Streetscape 2, Polk Street Streetscape 2a and 2b, Industrial Collector Streetscape, Collector Streetscape 2 and Tyler Street Project Edge.</p>
<b>STREET TREES (DESERT THEME)</b>			
	<ul style="list-style-type: none"> <li>• <i>Cercidium floridum</i></li> <li>• <i>Cercidium h. 'Desert Museum'</i></li> </ul>	<p>Blue palo verde Hybrid palo verde</p>	<p>Informal Canopy Streetscape for Urban Arterial Streetscape, Arterial Streetscape 2, Polk Street Streetscape 2a and 2b, Industrial</p>

Plant Form	Botanical Name	Common Name	Location
	<ul style="list-style-type: none"> <li>• <i>Cercidium praecox</i></li> <li>• <i>Chilopsis linearis</i></li> <li>• <i>Prosopis alba</i></li> <li>• <i>Prosopis chilensis</i></li> </ul>	<p>Sonoran palo verde Desert willow Argentine mesquite Chilean mesquite</p>	<p>Collector Streetscape, Collector Streetscape 2 and Tyler Street Project Edge.</p>
<b>ACCENT TREES</b>			
	<ul style="list-style-type: none"> <li>• <i>Albizia julibrissin</i></li> <li>• <i>Bauhinia variegata</i></li> <li>• <i>Jacaranda mimosifolia</i></li> <li>• <i>Lagerstroemia indica</i></li> <li>• <i>Pistacia 'Red push'</i></li> <li>• <i>Thevetia peruviana</i></li> <li>• <i>Citrus sp</i></li> </ul>	<p>Silk tree Purple orchid tree Jacaranda Crape myrtle Red push pistache Yellow oleander Orange</p>	<p>Palm Tree Streetscape for Major and Minor Entries, Intersection Details, Expressway Streetscape, Urban Arterial Streetscape, Arterial Streetscape 1, and Major Streetscape.</p>
<b>WINDROW</b>			
	<ul style="list-style-type: none"> <li>• <i>Eucalyptus species</i></li> <li>• <i>Oleander</i></li> </ul>	<p>Various Various</p>	<p>Edge and buffer conditions.</p>
<b>LARGE SHRUBS</b>			
	<ul style="list-style-type: none"> <li>• <i>Baccharis sarothoides</i></li> <li>• <i>Caesalpinea pulcherrima</i></li> <li>• <i>Cassia nemophylla</i></li> <li>• <i>Cistus purpureus</i></li> <li>• <i>Cistus ladaniferus</i></li> </ul>	<p>Desert broom Red bird of paradise Senna Orchid rock rose Crimson spot rock rose Hopseed bush</p>	<p>Throughout Streetscapes and Edge and Buffer conditions.</p>



Plant Form	Botanical Name	Common Name	Location
	<ul style="list-style-type: none"> <li>• <i>Dodonaea viscosa</i></li> <li>• <i>Fouquieria splendens</i></li> <li>• <i>Leucophyllum frutescens</i></li> <li>• <i>Liqustrum japonicum</i></li> <li>• <i>Nerium oleander</i></li> <li>• <i>Photinia fraseri</i></li> <li>• <i>Pittosporum tobira</i></li> <li>• <i>Pittosporum tobira</i></li> <li>• <i>'variegata'</i></li> <li>• <i>Xylosma congestum</i></li> </ul>	<p>Ocotillo</p> <p>Texas Ranger</p> <p>Japanese privet</p> <p>Petite Pink &amp; `Petite Salmon'</p> <p>Fraser photinia</p> <p>Mock orange</p> <p>Yellow oleander</p> <p>Shiny xylosma</p>	
<b>MEDIUM/SMALL SHRUBS</b>			
	<ul style="list-style-type: none"> <li>• <i>Agave shawii</i></li> <li>• <i>Aloe species</i></li> <li>• <i>Bougainvillea 'Oh La La'</i></li> <li>• <i>Carissa g.</i></li> <li>• <i>Encelia farinosa</i></li> <li>• <i>Euonymus japonica</i></li> <li>• <i>Euryops virides</i></li> <li>• <i>Ferocactus species</i></li> <li>• <i>Hemerocallis</i></li> <li>• <i>Hesperaloe parviflora</i></li> <li>• <i>Lantana camara</i></li> <li>• <i>Larrea tridentate</i></li> <li>• <i>Leucophyllum f.</i></li> <li>• <i>'Green Cloud'</i></li> <li>• <i>Muhlenbergia capillaries</i></li> <li>• <i>Muhlenbergia rigens</i></li> <li>• <i>Opuntia violacea</i></li> <li>• <i>Santolina virens</i></li> <li>• <i>Simmondsia chenensis</i></li> </ul>	<p>Shaw's century plant</p> <p>Aloe</p> <p>Bougainvillea</p> <p>Green Carpet</p> <p>Encelia</p> <p>Euonymus</p> <p>Green euryops</p> <p>Barrel cactus</p> <p>Daylily</p> <p>Red yucca</p> <p>Radiation lantana</p> <p>Creosote bush</p> <p>Texas Sage</p> <p>Regal Mist</p> <p>Deer Grass</p> <p>'San Rita' prickly pear</p> <p>Santolina</p> <p>Jojoba</p>	<p>Throughout Streetscapes and Edge and Buffer conditions.</p>
<b>GROUNDCOVER</b>			

Plant Form	Botanical Name	Common Name	Location
	<ul style="list-style-type: none"> <li>• <i>Acacia redonlens</i> 'prostrata'</li> <li>• <i>Baccharis pilularis</i></li> <li>• <i>Dalea capitata</i></li> <li>• <i>Lantana montevidensis</i></li> <li>• <i>Lantana</i> 'New Gold'</li> <li>• <i>Lantana sellowiana</i></li> <li>• <i>Lonicera japonica</i></li> <li>• <i>Santolina</i> <i>chamaecyparissus</i></li> <li>• <i>Trachelospermum</i> <i>jasminoides</i></li> <li>• <i>Verbena gooddingii</i></li> </ul>	<p>Trailing acacia 'Centennial' Sierra Gold Trailing lantana New Gold lantana Trailing lantana Honeysuckle Lavender cotton Star jasmine Sandpaper Verdena</p>	<p>Throughout Streetscapes and Edge and Buffer conditions.</p>
<b>VINES</b>			
	<ul style="list-style-type: none"> <li>• <i>Bougainvillea spectabilis</i></li> <li>• <i>Ficus pumila</i></li> <li>• <i>Lonicera japonica</i></li> <li>• <i>Rose banksiae</i></li> </ul>	<p>Various Creeping fig Hall's honeysuckle Lady Bank's rose</p>	<p>Throughout Streetscapes and Edge and Buffer conditions.</p>
<b>TURF</b>			
	<ul style="list-style-type: none"> <li>• <i>Hybrid bermuda</i></li> </ul>	<p>Tifgreen or 'Santa Ana'</p>	<p>Throughout Streetscapes and Edge and Buffer conditions.</p>

### 3.4.3 Entries

Entries or focal points are those key areas along the roadway where change in travel direction may occur. Often at these points a different land use may occur and the landscape should announce this change. Working in concert with signage, lighting and hardscape elements, the landscape plant materials will create "outdoor rooms" with an individual identity and level of importance that creates a unique mood as the motorist, pedestrian or cyclist passes through each space. Often a stoppage in movement occurs at these points; therefore, design elements that create a more lush effect of color, texture, and form should be concentrated at these locations.

#### Major Project Entries

The landscapes at the project entries along with the signage, lighting and hardscape elements will form a gateway into the Kohl Ranch and set the stage for the community identity. The landscape should draw views into the site and toward important development features. Tasteful plantings should indicate a transition from the surrounding land uses to the more formal roadways. Water resources should be concentrated toward irrigation of trees and shrubs. Ground covers may be of a more xeric nature.



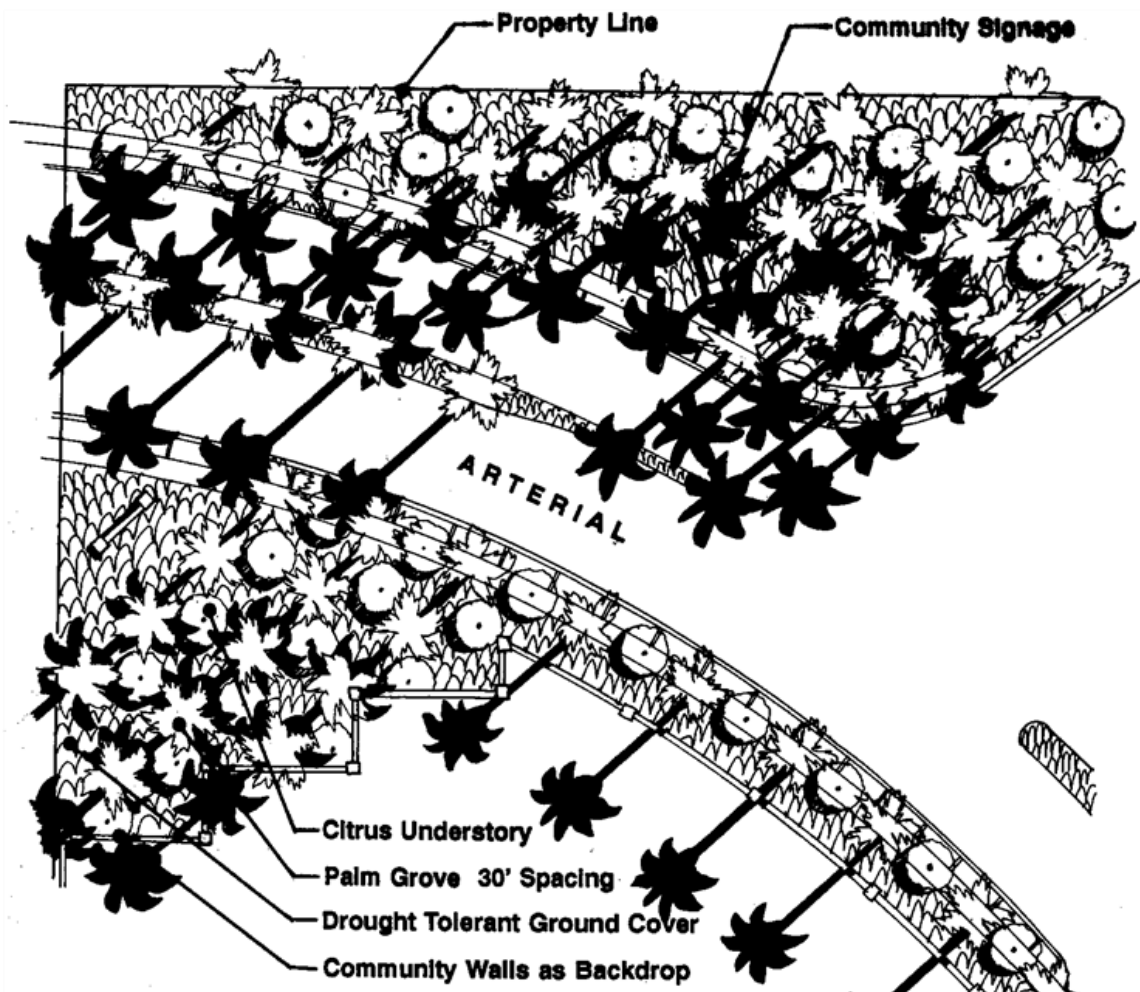
**Major Entry**

Major project entries are located at the following intersections:

- Avenue 60, north of "B" Street;
- Avenue 62 and Polk Street; and
- "E" Street and Avenue 66.

Major project entries will be landscaped with palm trees, with a maximum spacing of 30 feet on-center, with a citrus understory as designated in **Figure 3.4-3, Major Project Entry**. These plantings will be underlain by a drought tolerant groundcover. Community walls will create a backdrop for the entry landscape features.

**Figure 3.4-3, Major Project Entry**



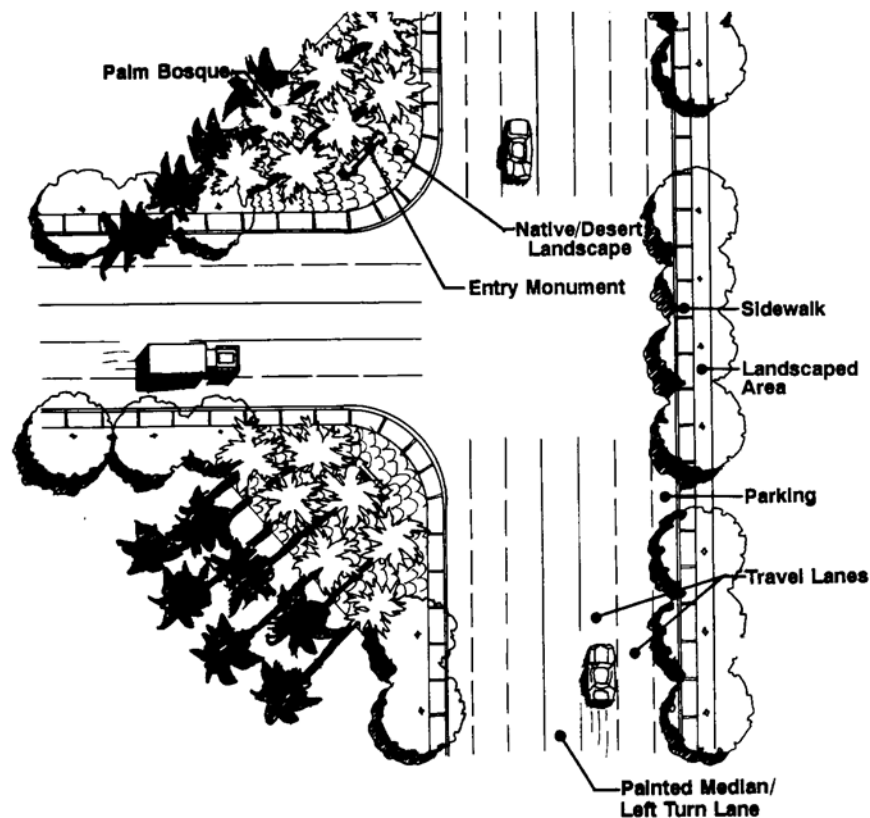
### Minor Entries

The minor entries should convey the same objectives as the major entries but on a less grand and more intimate scale. **Figure 3.4-4, Minor Entry**, depicts a typical minor entry at the following locations:

- "B" Street and Avenue 62;
- "F" Street and Tyler Street;
- "D" Street and Polk Street;
- the interior loop road and Polk Street; and
- Polk Street and boundary of Planning Areas J-4 and L-3.

The plantings which create the walls of the outdoor "room" should be pulled in and a greater sense of enclosure should be created. Elements of color, form, and texture should be highly developed for these intensely visible areas. Palm tree groves should be supplemented with native/desert landscape elements.

**Figure 3.4-4, Minor Entry**

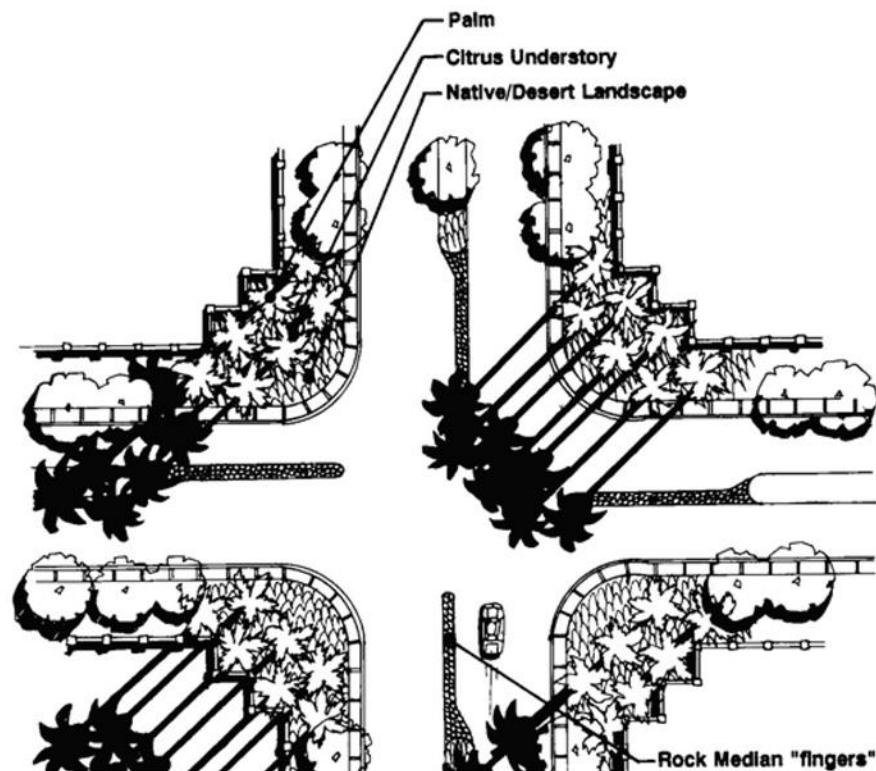


### 3.4.4 Intersections

Within the project site, certain key intersections will have special landscape treatments to denote the entry into a project neighborhood. This intersection treatment is illustrated in **Figure 3.4-5, Intersection Detail**. The landscape concept will be adapted to three-way intersections. Three-way intersections include the intersection of Avenue 62 and "C" Street, the intersection of "E" Street and Avenue 64, the intersection of "B" Street and Tyler Street, the intersection of "B" Street and Avenue 62, the intersection of "D" Street and Avenue 62, the intersection of "C" Street and Avenue 62, and the intersection of "E" Street and "F" Street. Intersections with special intersection treatments also include "F" Street and it turns south adjacent to Planning Area M-1B.

Major intersection landscapes should act as terminus points along the project roadways. They should appear as large enclosed outdoor rooms where the traveler can easily decide their direction of travel, react, then continue moving through the site. The edges of the room should be vertical to provide enclosure, with plant material located so that signage is emphasized. Color, form and texture should further define the importance of these decision points through an increased emphasis on water resources and plant materials.

**Figure 3.4-5, Intersection Detail**



**Note:** Landscape concept will be adapted to 3-way intersections.

### 3.4.5 Streetscapes

In order to achieve a cohesive overall circulation system for the Kohl Ranch and provide a strong community structure and identity, a consistent level of landscape design quality for public and private rights-of-way will be maintained. A consistent streetscape treatment will be implemented throughout the project as well as for the roadways surrounding the site. The general streetscape concept combines the use of palm trees with citrus understory, massed in selected locations, supported by turf and plantings of lush desert plants and annual color. The streetscapes are broken down into five major categories with special features and plant forms as indicated, and are applied to the hierarchy of the project roadways:

- Palm Tree Streetscape
- Formal Canopy Streetscape
- Informal Canopy Streetscape
- Windrow Planting
- Expressway Planting

Plant materials within these areas are intended to create a mood of movement and procession. Plant materials should direct and guide the traveler, screening sensitive views, or framing and creating focal points or "rooms" as the motorist, cyclist or pedestrian moves through the streetscape. Water resources should be minimized through the use of drip irrigation systems for trees and shrubs, and groundcovers which are attractive and thrive with limited irrigation or seasonal rains.

The five types of streetscape planting schemes listed above are applied to the hierarchy of project roadways which are broken down into seven roadway categories with special features and suggested plant forms as follows.



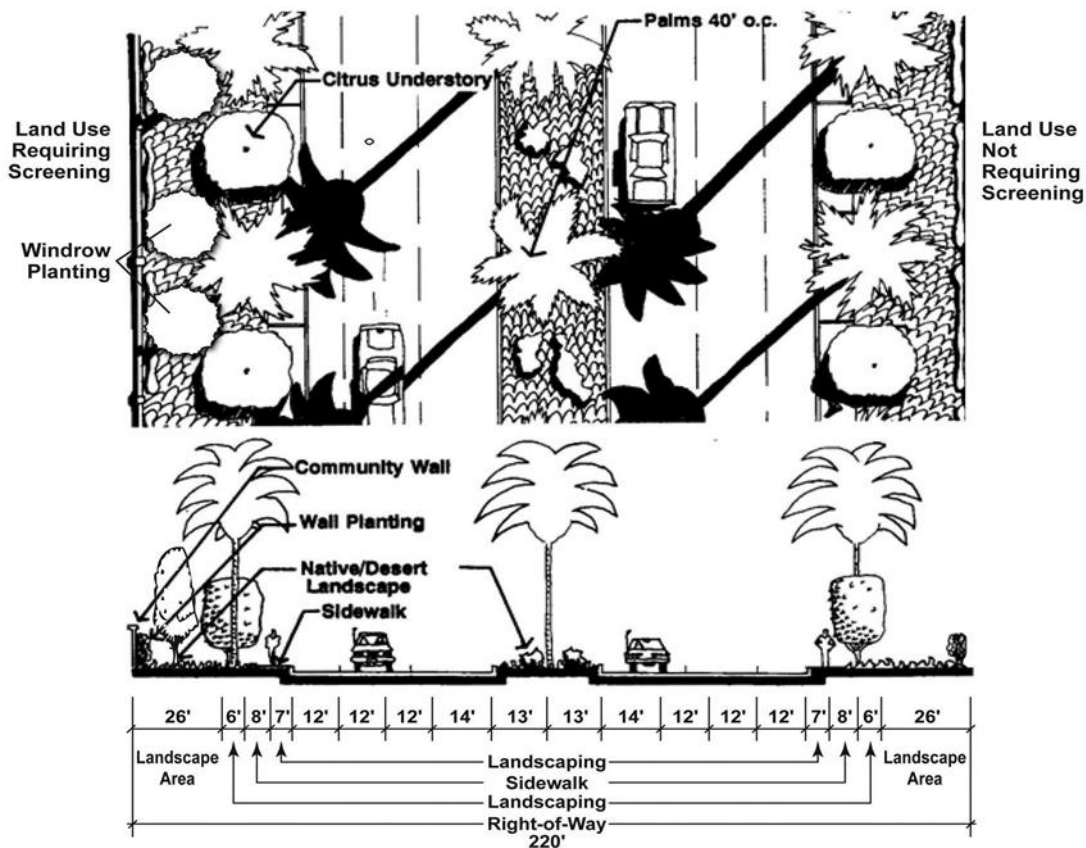
**Expressway Streetscapes**

**Expressway Streetscape (220' ROW)**

This streetscape applies to Avenue 62, the single project expressway, to serve as a formal progression through the site in the "grand allée" tradition of an estate roadway. The Palm Tree Streetscape is used as the basis for this streetscape, providing continuity across the project site. The Windrow Plantings are added at the rear of the ROW adjacent to land uses that may require screening such as Heavy Industrial or residential, while streetscapes in front of commercial uses and/or parks would not require screening. **Figure 3.4-6, Expressway Streetscape**, shows the concept for either situation.

- Where the expressway passes through the airport Emergency Touchdown Zone (ETZ), no trees should be planted.

**Figure 3.4-6, Expressway Streetscape**

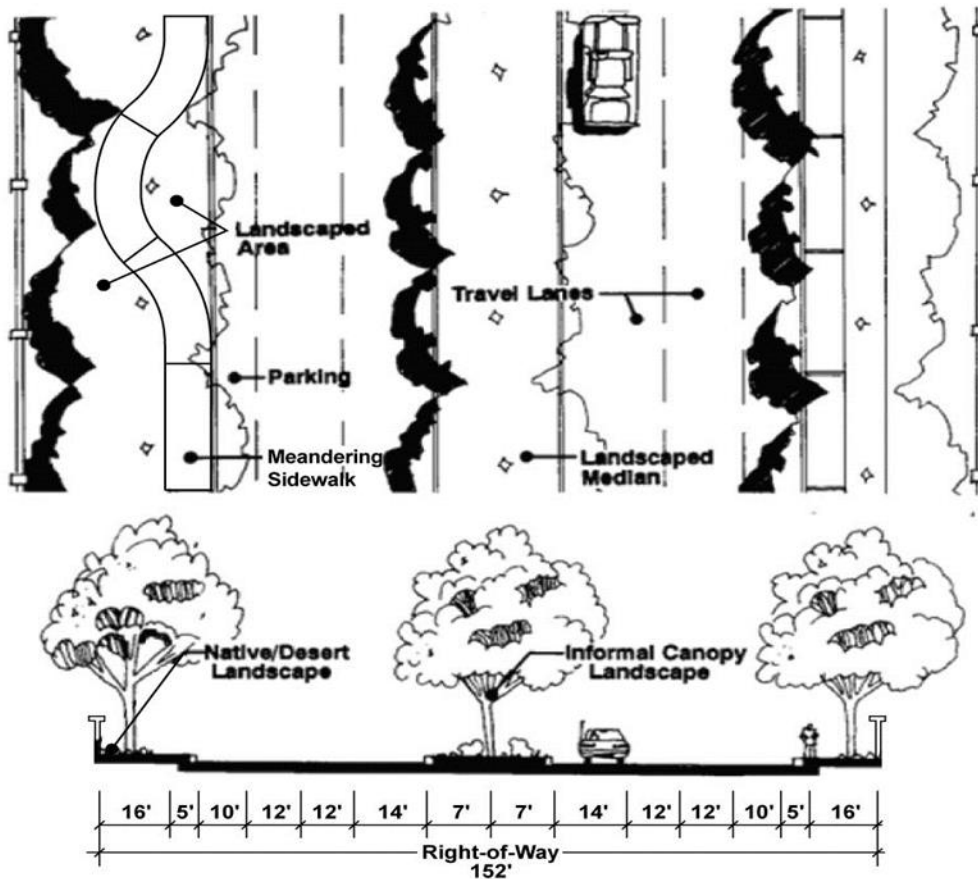




**Urban Arterial (152' ROW)**

This streetscape is intended for Avenue 66 only, and is interspersed with the Informal Canopy Streetscape, which should include trees that create a canopy by touching at their edges providing an overhead plane to impart a feeling of enclosure and to provide dappled shade as reflected in **Figure 3.4-7, Urban Arterial Streetscape**.

**Figure 3.4-7, Urban Arterial Streetscape**



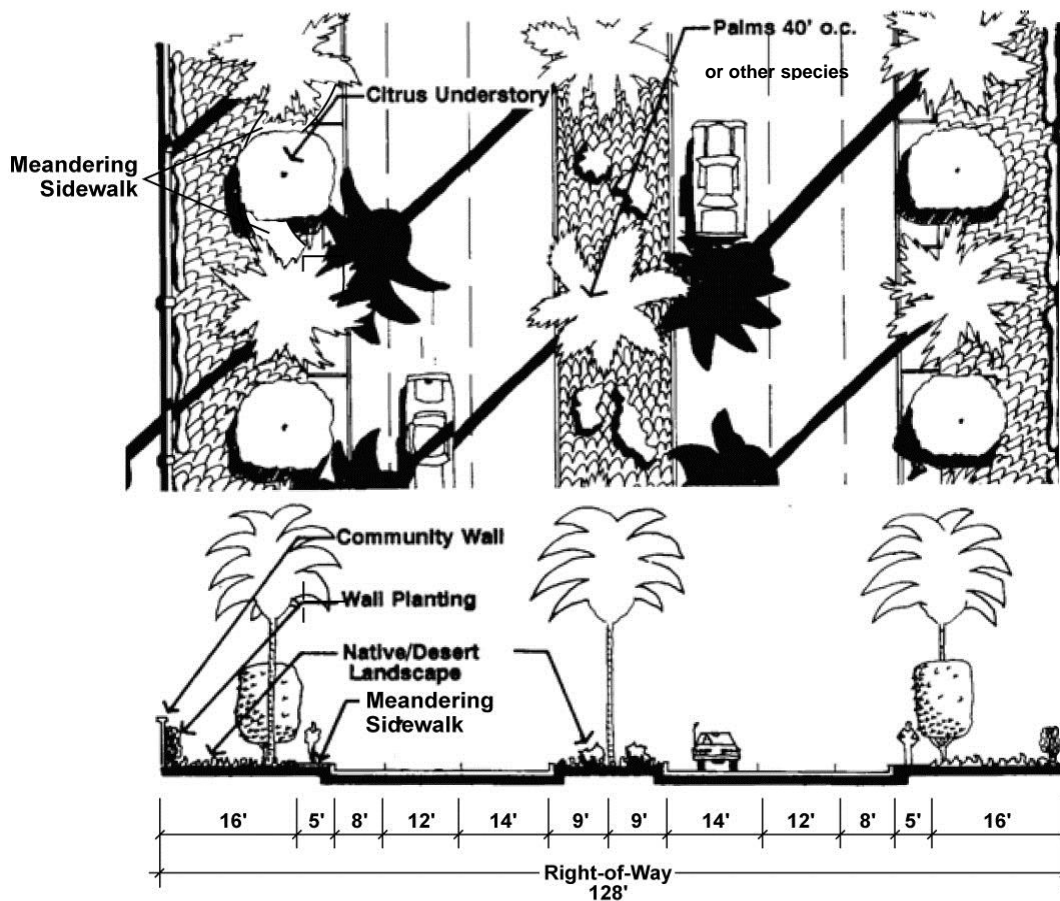
**Arterial Highway Streetscape (128' ROW)**

This streetscape is intended for Tyler Street north of Avenue 62, and Polk Street. Two different streetscapes are intended for these two arterials since Tyler Street is a major roadway within the project and Polk Street is along the edge of the project.

**Arterial Streetscape 1 –Tyler Street, north of Avenue 62**

- This roadway will include automobiles, bicycle and pedestrian traffic; therefore, foreground treatment and trees should be designed with regard to automobile speeds and/or the pedestrian experience depending on whether off-pavement trails are provided. The Palm Tree Streetscape is used as the basis for Arterial Streetscape 1. The Windrow Plantings are added at the rear of the ROW adjacent to land uses that may require screening such as Heavy Industrial or residential, while streetscapes in front of commercial or airport mixed uses would not require screening. **Figure 3.4-8, Arterial Streetscape 1**, shows the concept for either situation. Although palms may be used, other columnar tree types may also be used to better address pedestrian needs
- Arterial Streetscape 1 should include more formal elements near the entry focal points including a citrus understory to further emphasize the project entry.

**Figure 3.4-8, Arterial Streetscape 1**



**Arterial Streetscape 2 – Polk Street**

The Informal Canopy Streetscape is used as the basis for Arterial Streetscape 2. The Windrow Plantings are added at the rear of the ROW adjacent to land uses that may require screening such as Heavy Industrial, while streetscapes adjacent to the drainage canal also varies from a typical condition. **Figure 3.4-9, Arterial Streetscape 2 - Polk Street Streetscape 2A**, shows the typical situation. Polk Street, north of Avenue 62, would include Windrow Plantings, if screening is needed.

**Figure 3.4-9, Arterial Streetscape 2, Polk Street Streetscape 2A**

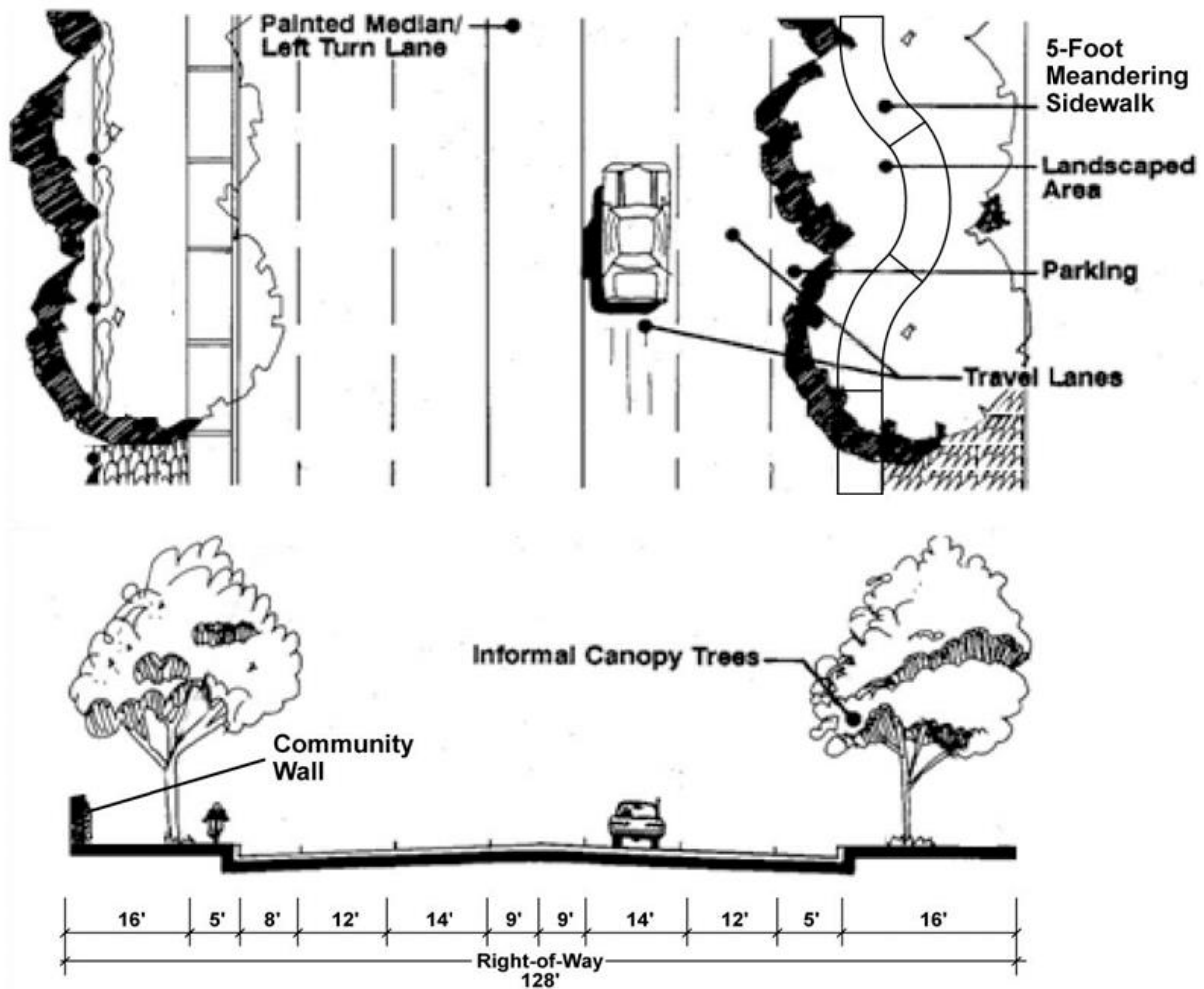
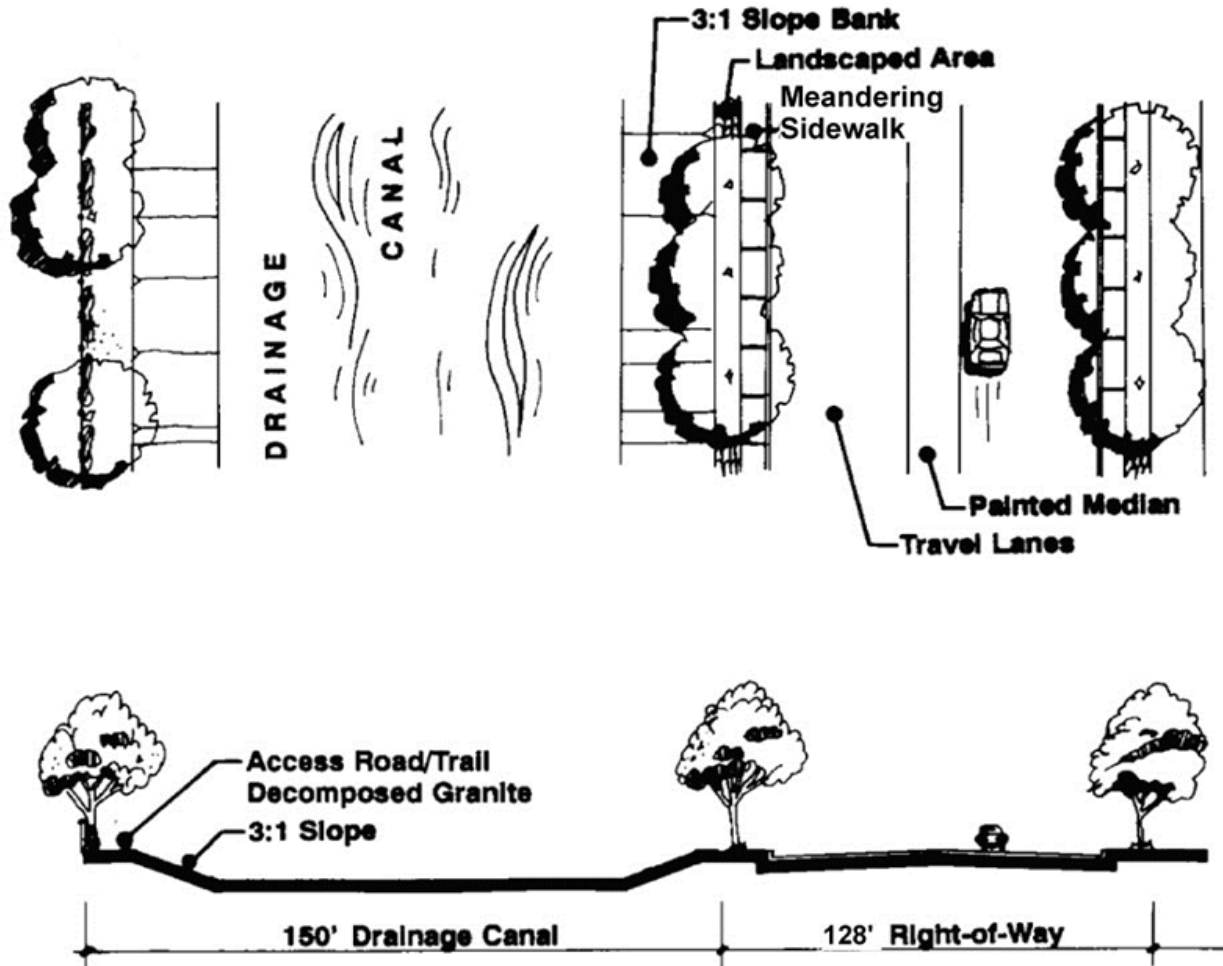


Figure 3.4-10, Arterial Streetscape 2, Polk Streetscape 2B depicts the project edge where the drainage facilities about Polk Street.

Figure 3.4-10, Arterial Streetscape 2, Polk Street Streetscape 2B



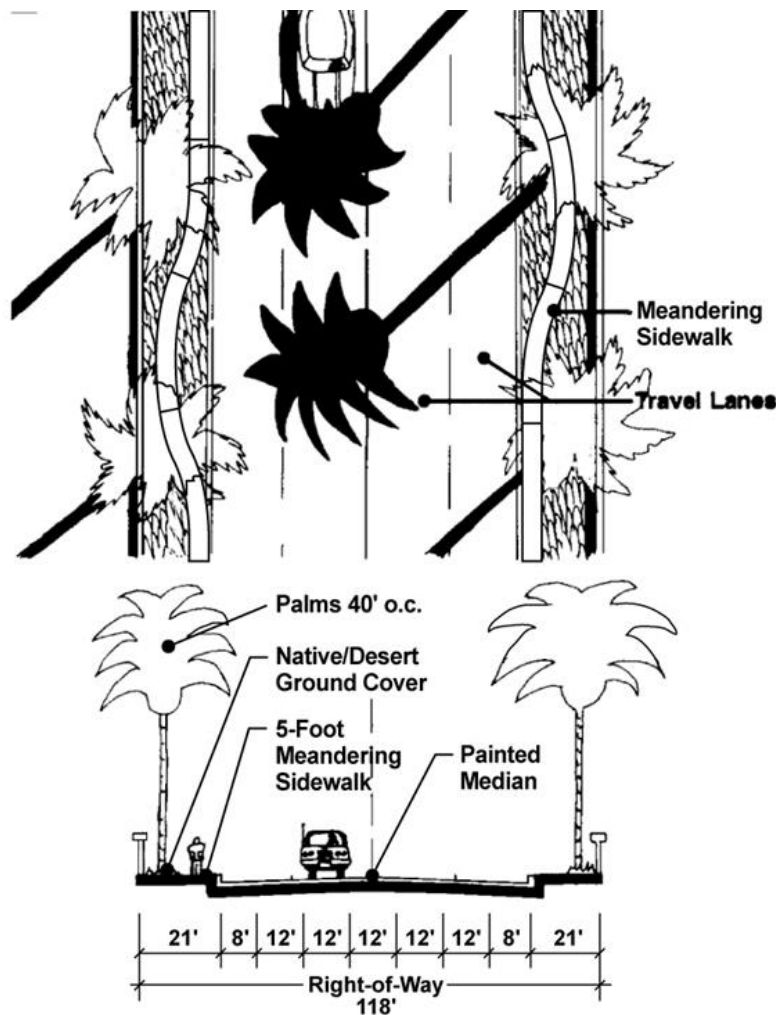
**Major Highway Streetscape (118' ROW)**

This streetscape is intended for Avenue 64 and "E" Street. Two different streetscapes are intended for these two majors since Avenue 64 is bordered by the utility corridor and evacuation channel while "E" Street meanders through residential areas.

**Major Streetscape 1 – Avenue 64**

The Palm Tree Streetscape is used as the basis for **Figure 3.4-11, Major Streetscape 1**. This streetscape is intended for Avenue 64 as it traverses the project site. Palm trees provide a vertical element that denote the important roads through the site, and should be spaced at 40 feet on center with a native/desert ground cover. Avenue 64 is flanked by the utility corridor and the evacuation canal so screening of these areas is appropriate by adding Windrow Planting at the back of the ROW to provide a pleasant driving experience through the project area.

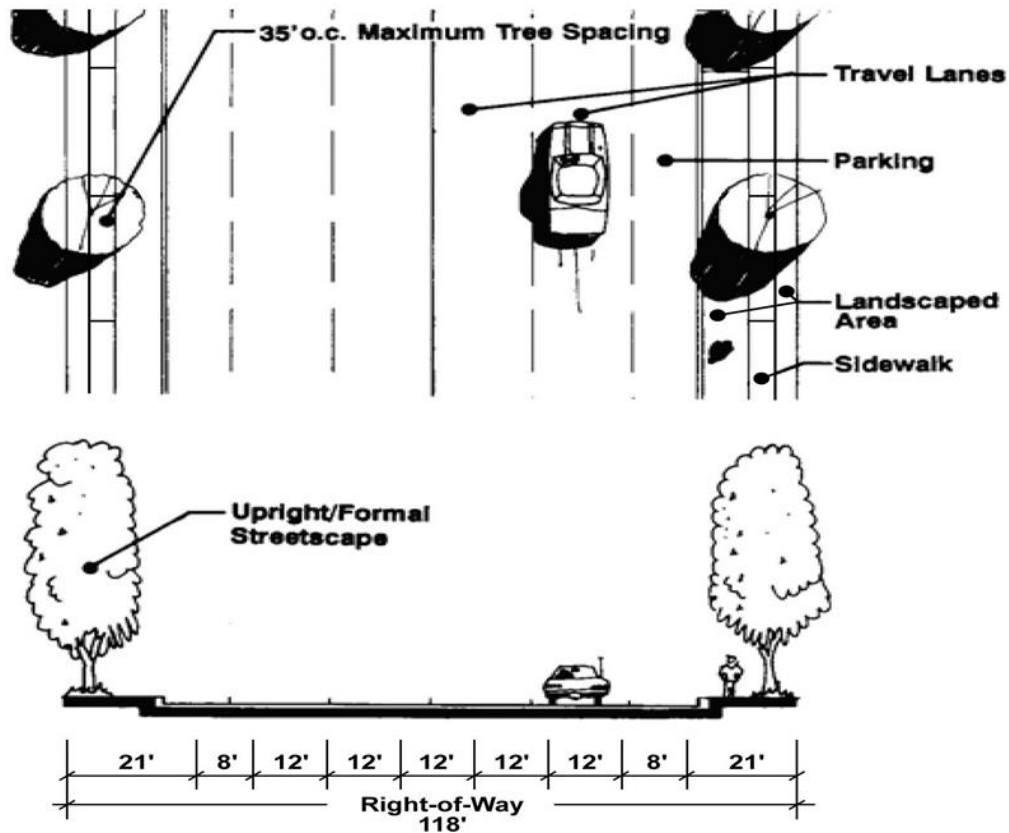
**Figure 3.4-11, Major Streetscape 1**



**Major Streetscape 2 – “E” Street**

**Figure 3.4-12, Major Streetscape 2**, illustrates the Formal Canopy Streetscape, using upright trees, with a maximum spacing of 35 feet on center to shade the sidewalk area. This streetscape should be used for “E” Street.

**Figure 3.4-12, Major Streetscape 2**



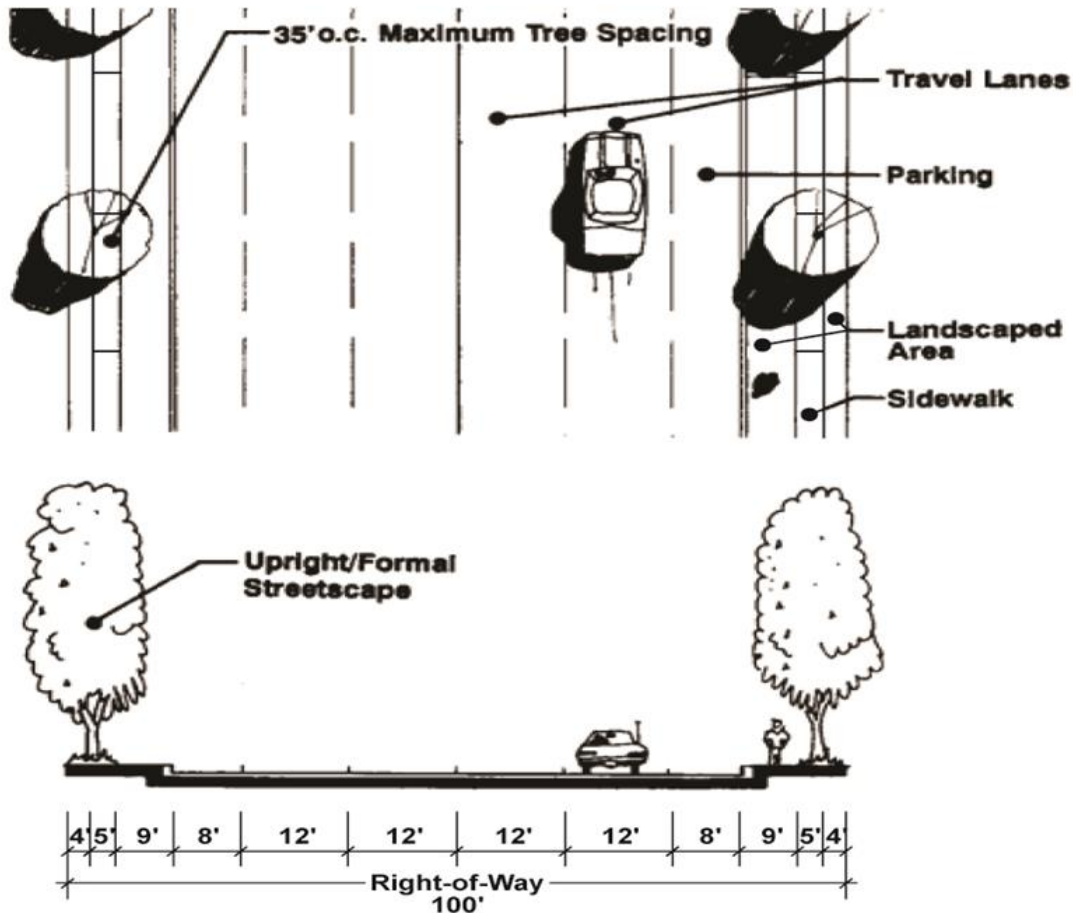
**Secondary Streetscape (100' ROW)**

This roadway treatment is intended be the next progression from an automobile dominated streetscape to a more bicycle and pedestrian-oriented streetscape. Design speeds along this roadway will be slower, views from passing motorists will be longer, and occasional bicyclists and pedestrians will be present, therefore plant material color and texture will be of importance along this roadway. Just as along the expressway and arterial streetscape, the plant material will open and close to provide or control the view along the roadway. Three streetscapes are proposed for the Secondary Highways in the Kohl Ranch Specific Plan area.

**Secondary Streetscape 1**

**Figure 3.4-13, Secondary Streetscape 1**, illustrates the Formal Canopy Streetscape, using upright trees, with a maximum spacing of 35 feet on center to shade the sidewalk area. This streetscape should be used for "C" Street and Tyler Street, north of Avenue 64.

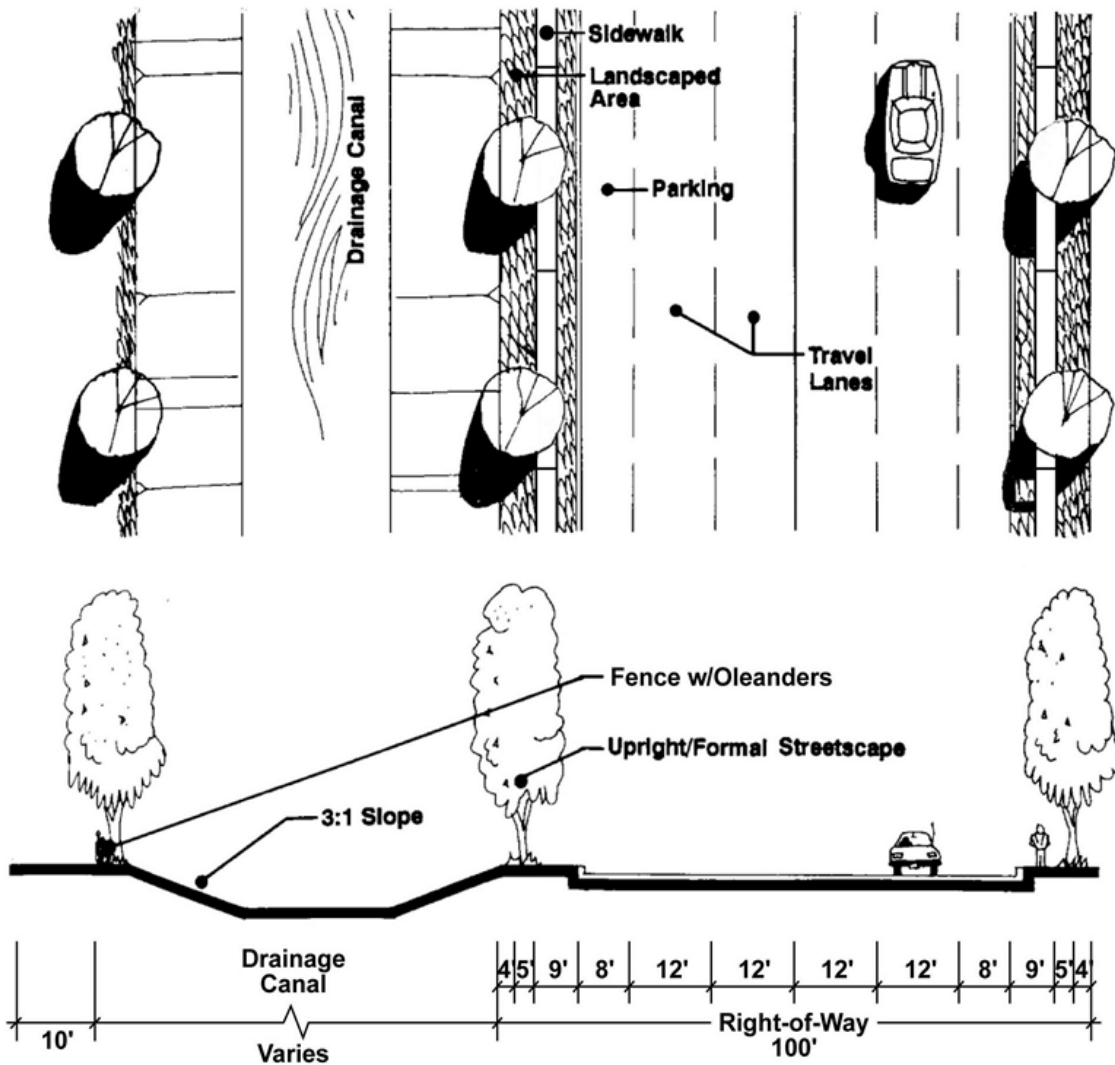
**Figure 3.4-13, Typical Secondary Streetscape 1**



**Secondary Streetscape 2**

**Figure 3.4-14, Secondary Streetscape 2** illustrates the Formal Canopy Streetscape alongside the drainage channel, which could occur along Tyler Street, north of Avenue 64. Upright trees are used at the edge between the roadway and the 3:1 side slope of the drainage facilities.

**Figure 3.4-14, Secondary Streetscape 2**



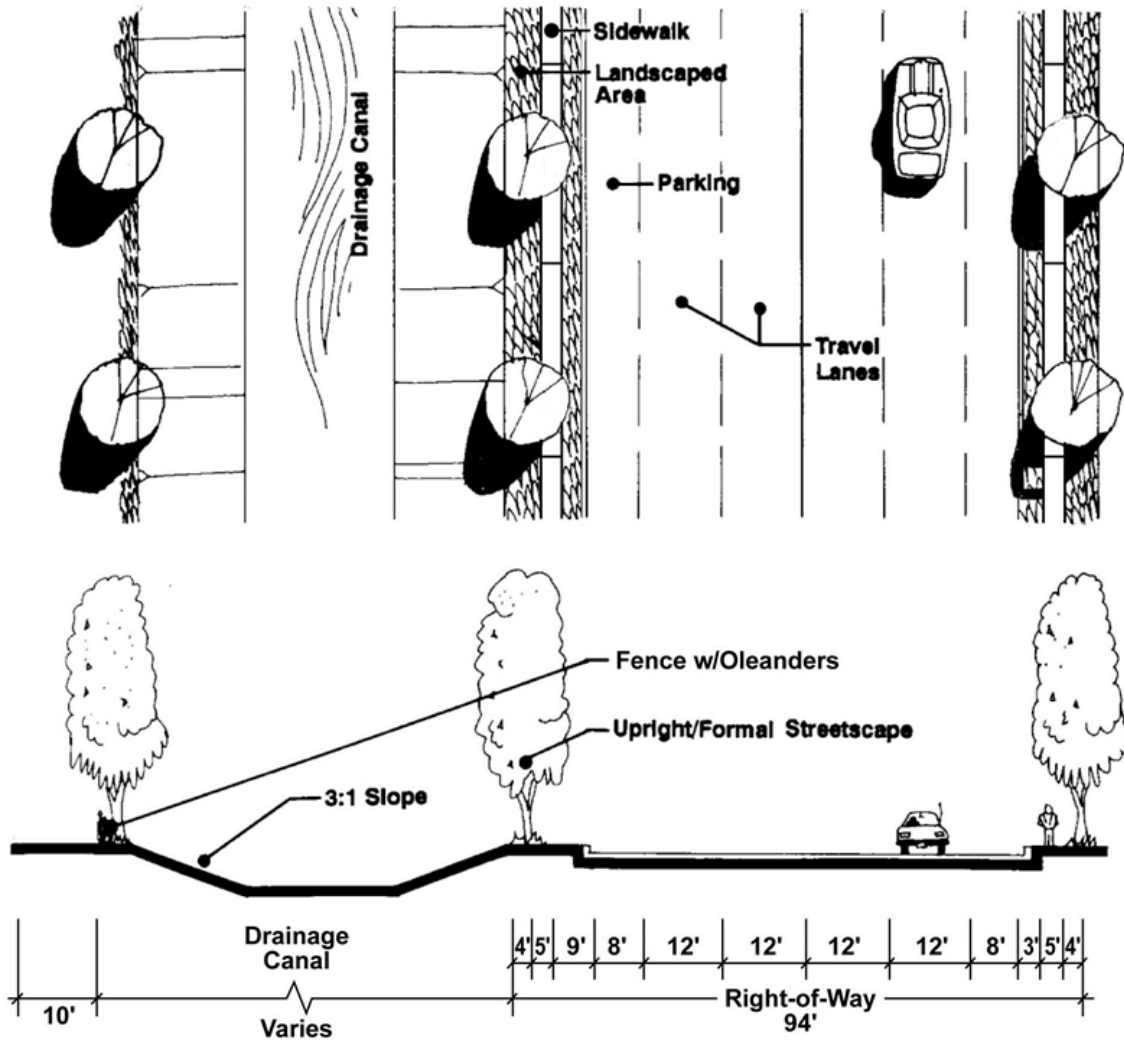
**Note: Typical channel section - widths may vary.**



**Modified Secondary Streetscape**

Tyler Street is also a secondary highway with a modified cross section south of Avenue 64. **Figure 3.4-15, Modified Secondary Streetscape 2** illustrates the Formal Canopy Streetscape alongside the drainage channel, which could occur along Tyler Street. Upright trees are used at the edge between the roadway and the 3:1 side slope of the drainage facilities.

**Figure 3.4-15, Modified Secondary Streetscape 2**

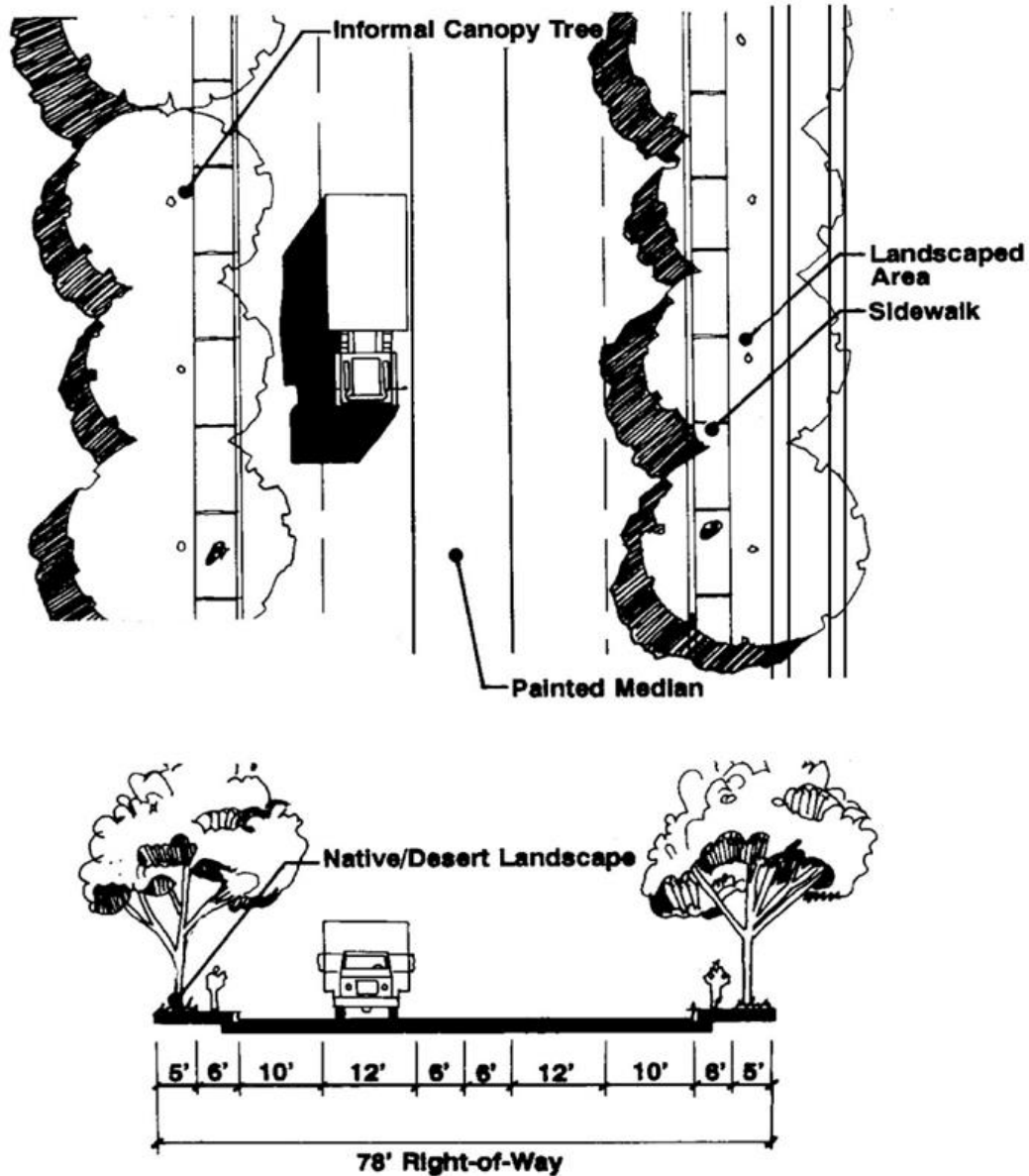


**Note: Typical channel section - widths may vary.**

**Industrial Collector Streetscape (78' ROW)**

The streetscape proposed for the Industrial Collector roadway classification is shown on **Figure 3.4-16, Industrial Collector Streetscape**, which illustrates Avenue 60 adjacent to heavy industrial and airport mixed use areas. Informal canopy trees should be used to frame the roadway, augmented by native/desert ground cover. A painted median is proposed.

**Figure 3.4-16, Industrial Collector Streetscape**



**Collector Streetscape (74' ROW)**

Collector roadways provide access to residential areas beyond the arterial roadways. Pedestrian and bicycle uses will be of a major concern in the development of the landscape plan for these intimate streetscapes. Front yard landscapes may be increased along this low speed roadway to create a more detailed streetscape of interesting forms, colors, textures, odors and sounds. Residential units front this roadway and access to driveways and compatibility of right-of-way landscapes to residential front yard landscapes must be carefully integrated. Two streetscapes are proposed for the Collector Streets within the Kohl Ranch Specific Plan area.

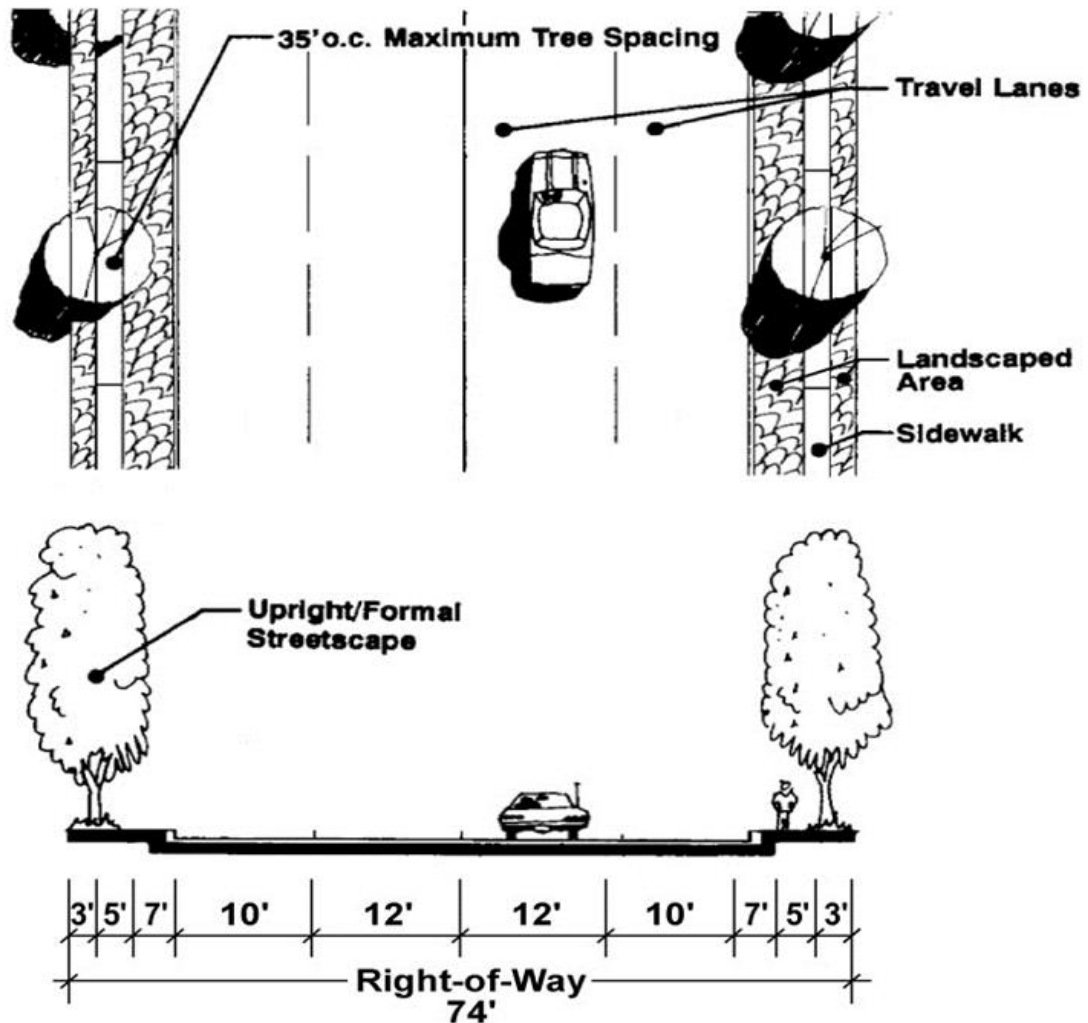


***Residential Collector***

**Collector Streetscape 1**

**Figure 3.4-17, Collector Streetscape 1** illustrates the landscaping for "B" Street and Tyler Street north of Avenue 66, adjacent to Planning Area M-4. The Formal Canopy Streetscape is incorporated in Collector Streetscape 1, using upright trees, with a maximum spacing of 35 feet on center to shade the sidewalk area.

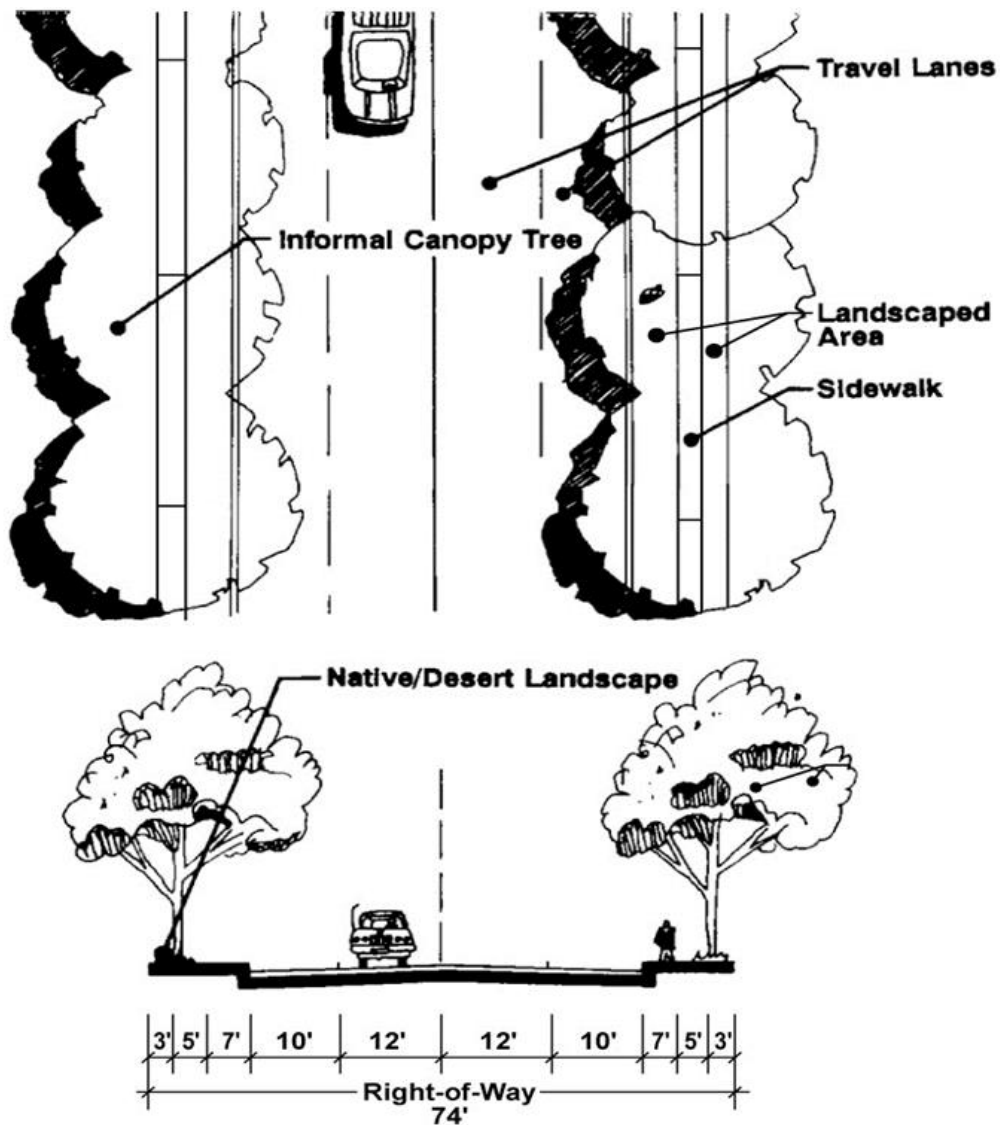
**Figure 3.4-17, Collector Streetscape 1**



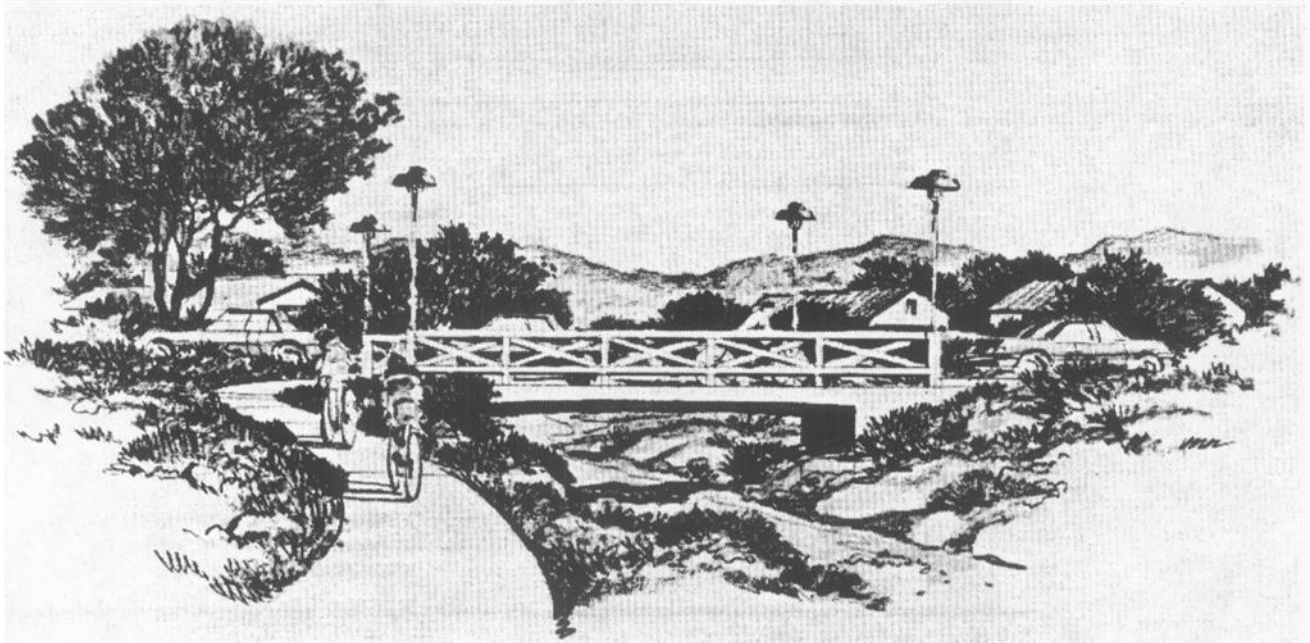
**Collector Streetscape 2**

The second collector streetscape is utilized on the more interior loop roads within the residential neighborhoods. It should be developed with the Informal Canopy Streetscape shown in **Figure 3.14-18, Collector Streetscape 2**.

**Figure 3.14-18, Collector Streetscape 2**



### 3.4.6 Paths and Trails



#### **Paths and Trails**

##### **Project Trails**

The Kohl Ranch shall have improved trails which will access the extensive open space and recreation areas of the site, and connect with the county trail system. The trails should be incorporated into the project drainage network, and should connect residential areas with schools, commercial areas, public facilities and parks. Improved trails dimensions and surface material should be governed by the County of Riverside standards for construction. Special landscape statements should be created for locations where trail segments intersect, and especially where they cross roadways. Boulders can be used to call attention to trail crossings and intersections, and can function as seats. A large shade tree also should be provided.

##### **Bicycle Paths**

Bicycle trails may be incorporated as a portion of the project roadways. Mountain bicycles should not be allowed off-road except on improved trails.

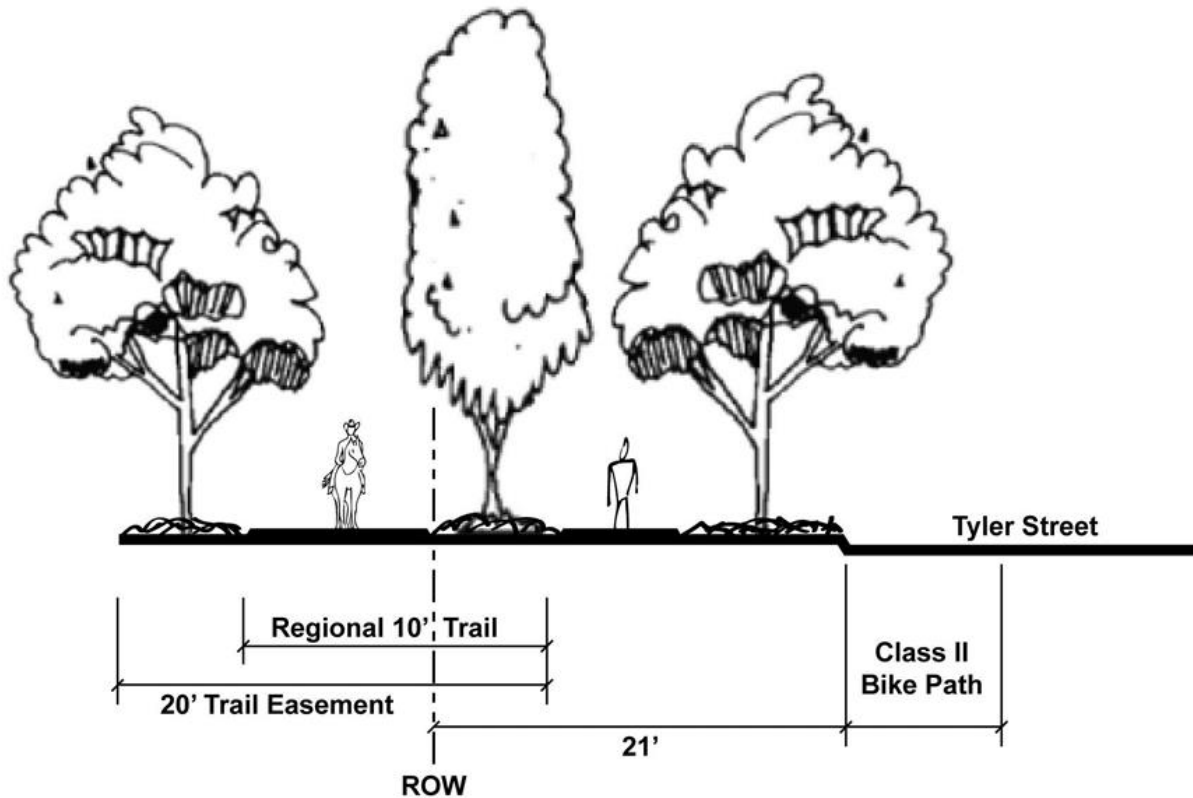
##### **Tyler Street Trail**

Tyler Street will contain both a Class II Bike Path and Regional Trail connection between Avenue 60 and Avenue 62 on the east side of Tyler Street. Regional trails are the main primary long distance trails within the County and are typically designed to provide linkages between communities, regional parks, and open space areas. Regional trails are designed to serve users needing soft trail surfaces, including equestrians, pedestrians, joggers, and mountain bikers and should consist of decomposed granite.

Class II Bike Paths provide a striped lane for one-way bike travel on a street or highway.

The Class II Bike Path along Tyler Street shall be a striped lane located within the right-of-way on pavement as illustrated in **Figure 3.4-19, Tyler Street Trail** or as agreed to by the Riverside County Regional Parks and Recreation District.

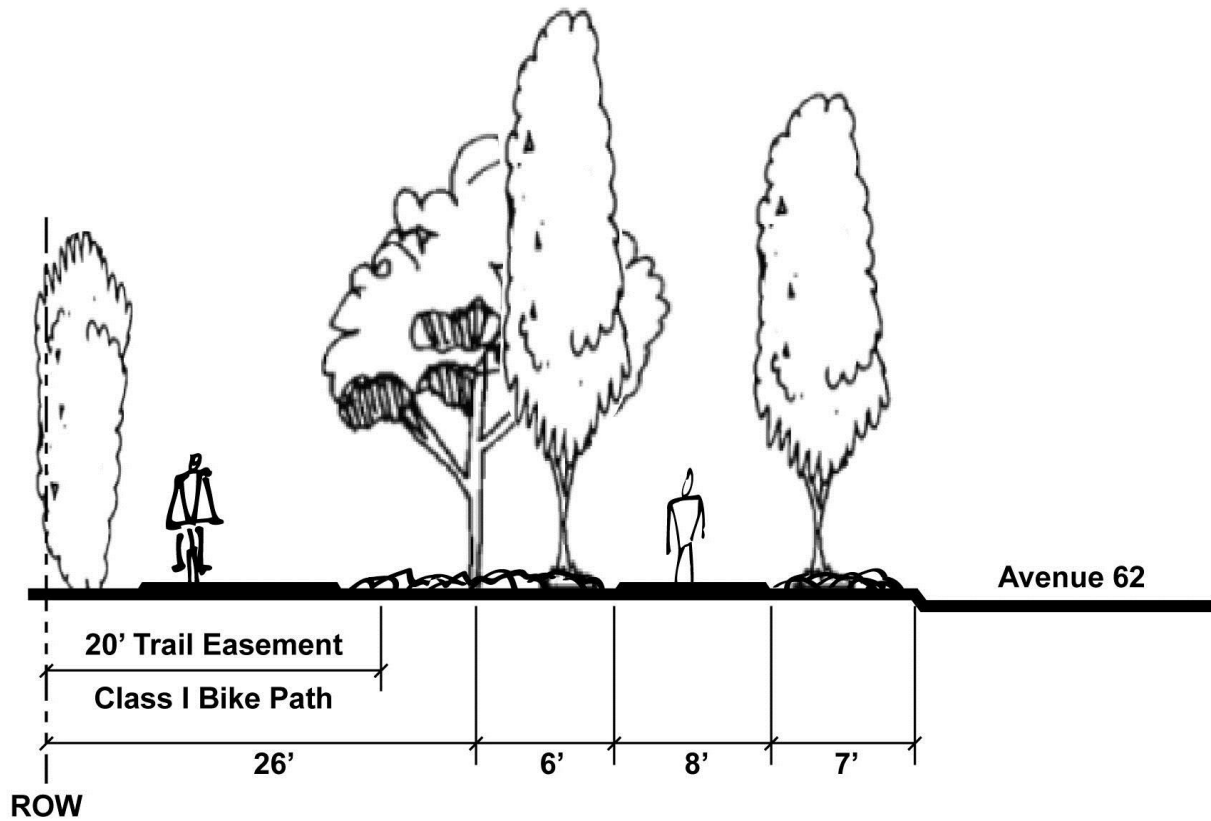
**Figure 3.4-19, Tyler Street Trail**



**Avenue 62 Trail**

Avenue 62 will provide for a Class I Bike Path on the north side of Avenue 62. Generally speaking, Class I Bike Paths are located in an easement outside the right-of-way. In the case of Avenue 62, however, the Class I Bike Path shall be located within the right-of-way as illustrated in **Figure 3.4-20, Avenue 62 Trail**.

**Figure 3.4-20, Avenue 62 Trail**





### 3.4.7 Edges

Landscape edges are the landscape treatment areas that create an envelope around roadways and development parcels, softening and buffering their harsh effects upon the landscape. These landscapes are very important in developing the community theme spilling over into residential and roadway landscape treatments. Resources of water and plant materials should be minimized in these areas with the intent of forming landscapes which require minimal maintenance. These areas additionally may be used as a continuation of adjacent passive recreation or open space areas.

Formal landscape treatment areas are intended to establish an organized, structured and even at times homogeneous landscape that is characteristic of more ordered forms. These treatment areas should appear lush and should contrast rather than blend with the natural desert landscape. The plant material should be used as a tool for space articulation, emphasizing topography and creating overt spatial relation statements. A grove should be visualized as the ultimate intent of treatment when designing these rural themed landscapes. Further, areas that are in direct contact with development areas may include plant materials which will foster a sense of security and privacy while maintaining views. Plant materials along these contact points may be slightly taller to screen and buffer the effects of rooflines and building facades.

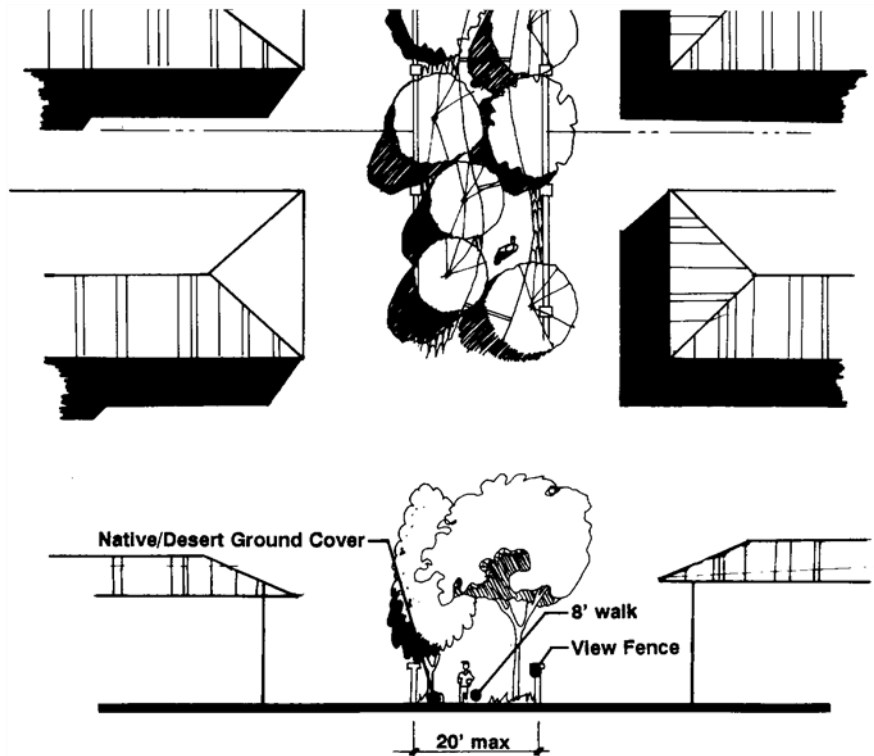
Natural landscapes composed of xeric (drought resistant) plant species are intended to blend with the existing landscape in color, form and texture. These landscapes occur along the project edges and drainage corridors, and will require a minimum of water resources. Only sporadic irrigation should be used to establish growth and little or no irrigation should be required after maturity. They should be organically arranged, reflecting the growth patterns of existing vegetation. Their primary intent is to screen and buffer residential developments.

- Landscape plantings are used as screens to block views, create microclimates, or as architectural space articulation elements. Plant materials should be thick and full, defining an edge of space or thick elongated masses. These screens may occur along the edges of development or within development parcels.
- Small structures can be easily screened with shrubbery, larger two-story structures can be buffered through the use of vertical trees. All plants should be chosen from the plant matrix. Care should be taken when selecting plants to consider their size, so that desirable views are not blocked.
- In certain cases, proper screening may be more easily achieved by building a wall or fence. Material selection must be compatible with the adjacent architecture. It is encouraged to soften walls and fences with vines and shrubs. Combinations of tree masses will be effective in screening non-desirable views from one area to another. Shrub masses can also help when placed correctly.

**Windrow/Trail**

Windrows are used to define land use areas, provide a wind break, visually screen road and/or uses from one another, and to structure and enclose pedestrian trails throughout the residential neighborhoods. The Windrow/Trail Detail in **Figure 3.4-21, Windrow/Trail Detail** illustrates this landscape edge.

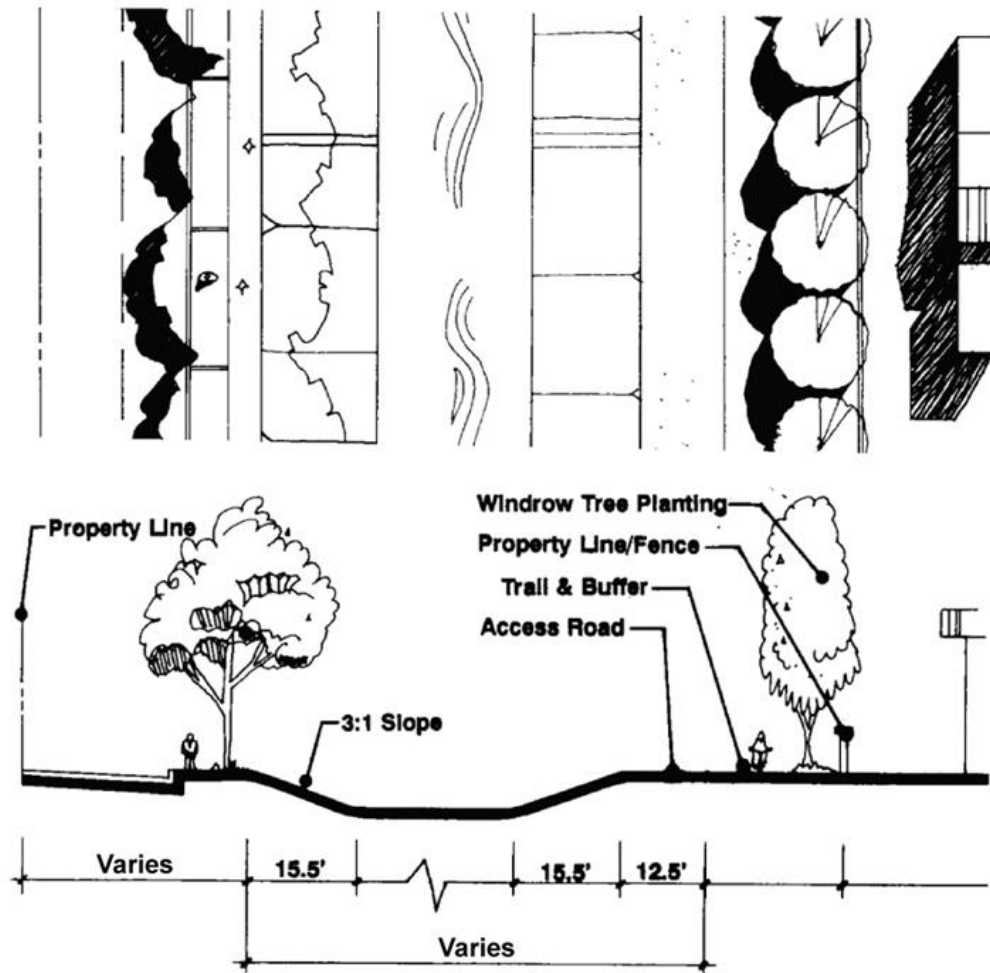
**Figure 3.4-21, Windrow/Trail Detail**



**Drainage/Project Edge**

Where drainage facilities are proposed along the Kohl Ranch property boundary, adjacent to residential uses, a special edge treatment is needed. **Figure 3.4-22, Project Edge** illustrates this condition, and the use of windrow plantings to buffer the residences.

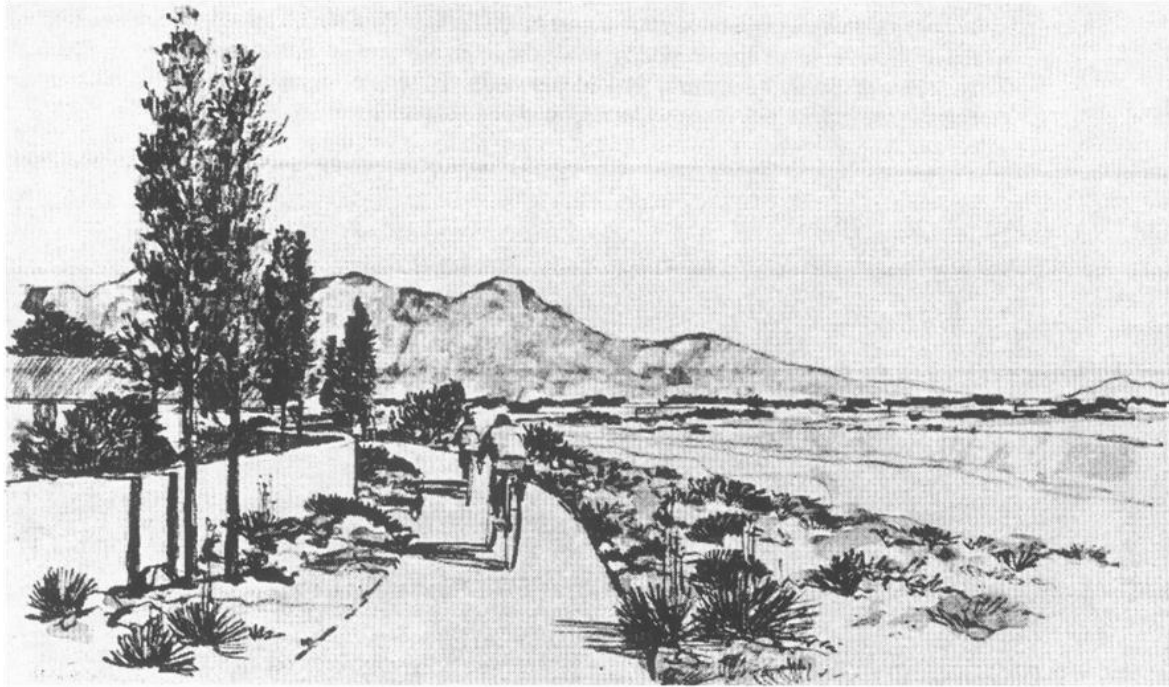
**Figure 3.4-22, Project Edge**



**Note: Typical channel section - widths may vary.**

### Drainage Canal Edge Conditions

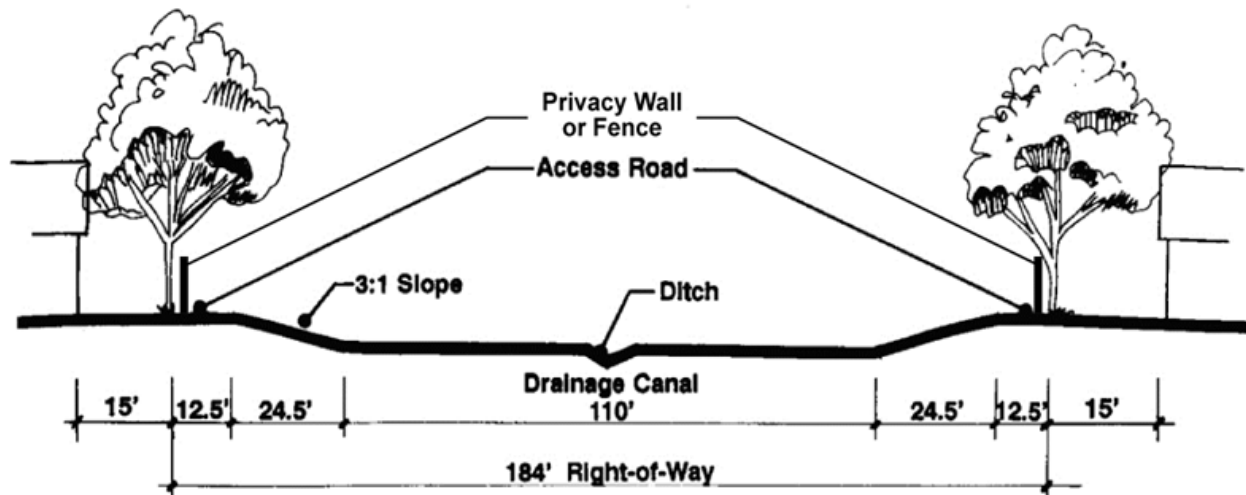
In a number of instances, through the Kohl Ranch site, drainage facilities are integrated into residential neighborhoods, commercial areas, mixed use and the Mixed Use/Air Park center.



*Drainage Channel and Trail*

The edge condition created by these relationships is shown in **Figure 3.4-23, Drainage Canal Edge Condition.**

**Figure 3.4-23, Drainage Canal Edge Condition**



**Major Recreation Use – Golf Course or Race Track**

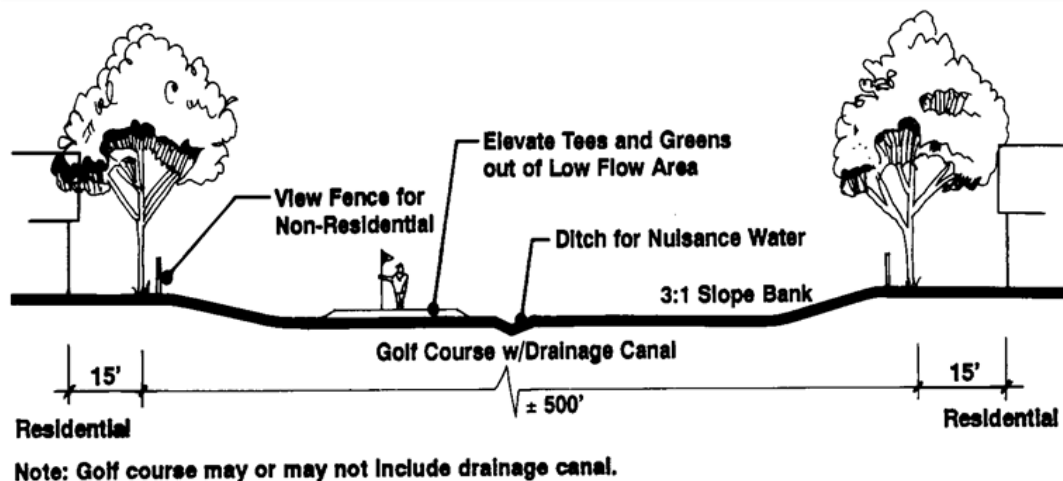
If a golf course alternative is developed, the approximately 500-foot section (widths may vary) should include an area of elevated trees and greens out of the low flow area, and a ditch for nuisance water. A view fence should be placed along the interface with non-residential uses.



**Golf Course Edge**

If a race track is developed, edges adjacent to roads and non-race track uses need to be buffered for both aesthetic purposes and to reduce noise from the race cars as depicted in Figure 3.4-24, Golf Course/Racetrack Edge Condition.

**Figure 3.4-24, Golf Course/Racetrack Edge Condition**



### 3.4.8 Buffers

Landscape treatments are used to buffer adjacent land uses within the Kohl Ranch Specific Plan area. These landscape treatments should be a thoughtful blending of edges of plant materials from one treatment type to another. The transitions may be abrupt or a slow fading effect, depending on the desired effect.

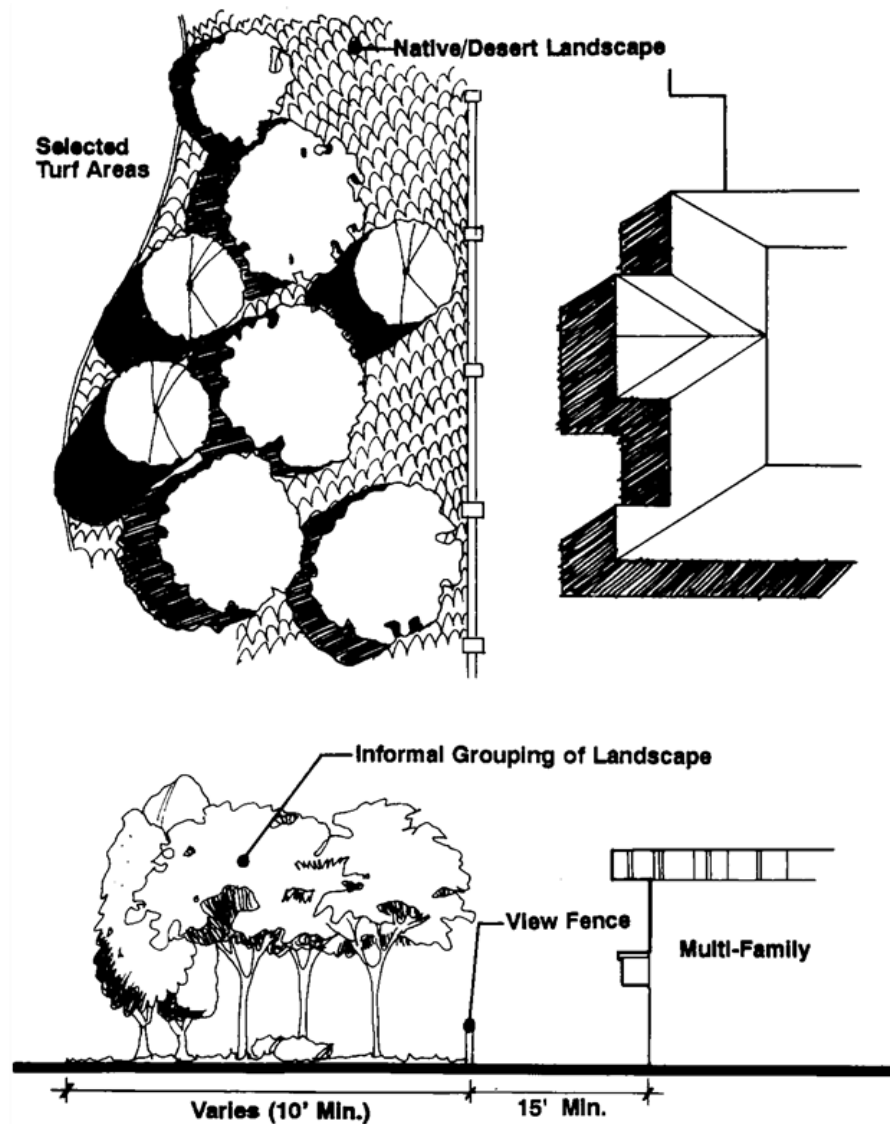
- In order to provide privacy from one unit to another, it is encouraged that vertical tree masses be planted along side yards slopes where practical.
- Human use of natural open space areas will be accommodated by a limited number of trails and parks. These areas shall consist of native and xeric plant materials which shall be retained in their natural condition with no water, other than natural rainfall.
- Maintenance will consist of only occasional trail maintenance. This landscape will serve as a visual amenity for the community, and will also buffer adjacent uses outside the Specific Plan area.

If the race track is developed, buffers may include fences or walls, landscaping used to screen views, berms, buildings, a combination of the preceding, or other means as may be acceptable to the County and/or needed to reduce noise. Concepts for buffer edge conditions shall be submitted with plot plan/site plan for the race track use.

**Park**

Special landscape treatments are recommended where residential dwellings abut the high-use local parks. An informal landscape grouping at the park edge, with native/desert groundcover should be planted along the view fence, to ensure privacy and to aid in noise attenuation. A 15-foot minimum rear yard setback is recommended between the residential building and the view fence as depicted in **Figure 3.4-25, Park Buffers**.

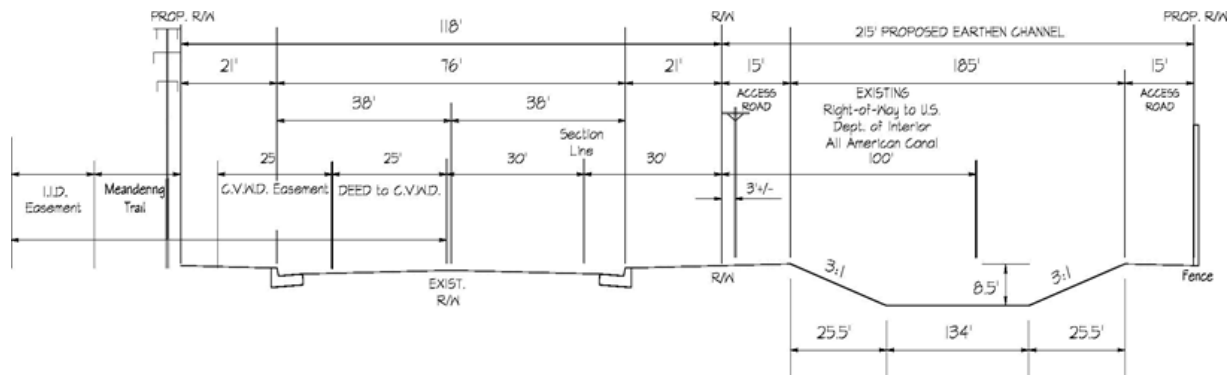
**Figure 3.4-25, Park Buffers**



**Evacuation Channel and Utility Easement Beltway**

The Avenue 64 Evacuation Channel runs across the Kohl Ranch site in an east-west direction, between Tyler and Polk Streets. At its proposed final elevation, the channel should be lower than the adjacent land on either side of the channel, screening the concrete bottom from view as reflected in **Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**. The Evacuation Channel is within the utility easement beltway which also runs east-west through the site. This beltway contains easements for CVWD irrigation pipelines, the channel, and a power line easement for an existing 161 kV power line. This area will remain in open space and will incorporate a segment of the project-wide trail system. Residential land uses should be clustered to the north of the beltway, to preserve this area as a common open space amenity.

**Figure 3.4-26, Evacuation Channel Buffer and Utility Easement Beltway**

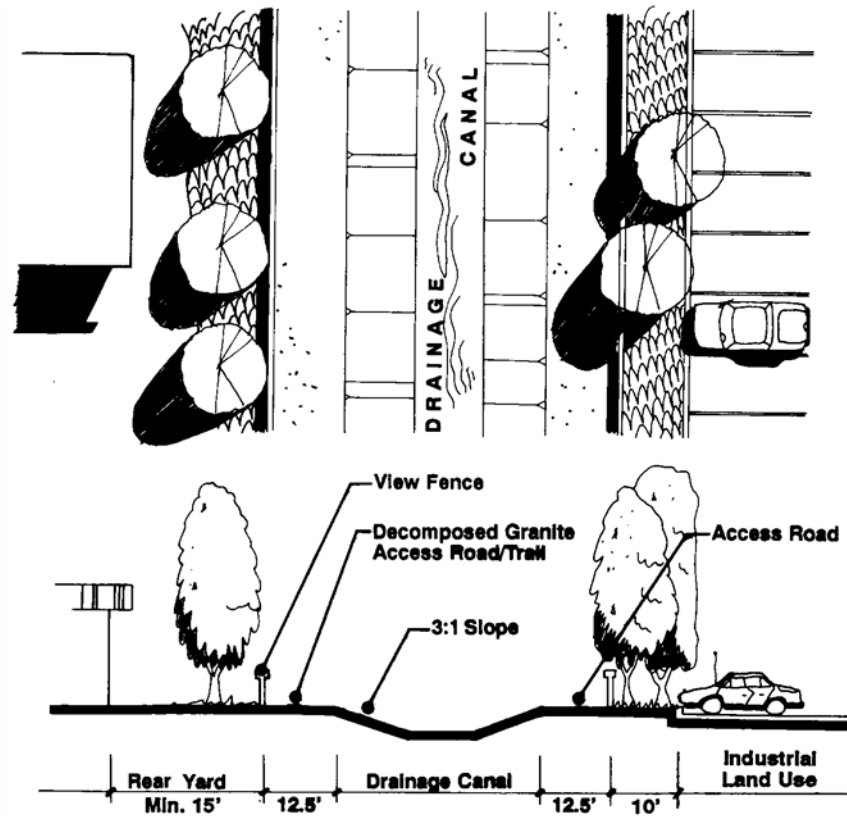




**Drainage Channel**

Drainage channels are located throughout the Kohl Ranch, to handle storm flows and to provide a linkage between park and open space amenities. The proposed buffers for these drainage facilities are indicated in **Figure 3.4-27 Channel Buffer Detail**. As indicated, informal plantings are recommended adjacent to the view fence to screen adjacent residential and non-residential land uses. Trails should be sited along the top of the 3:1 side slope and should consist of decomposed granite.

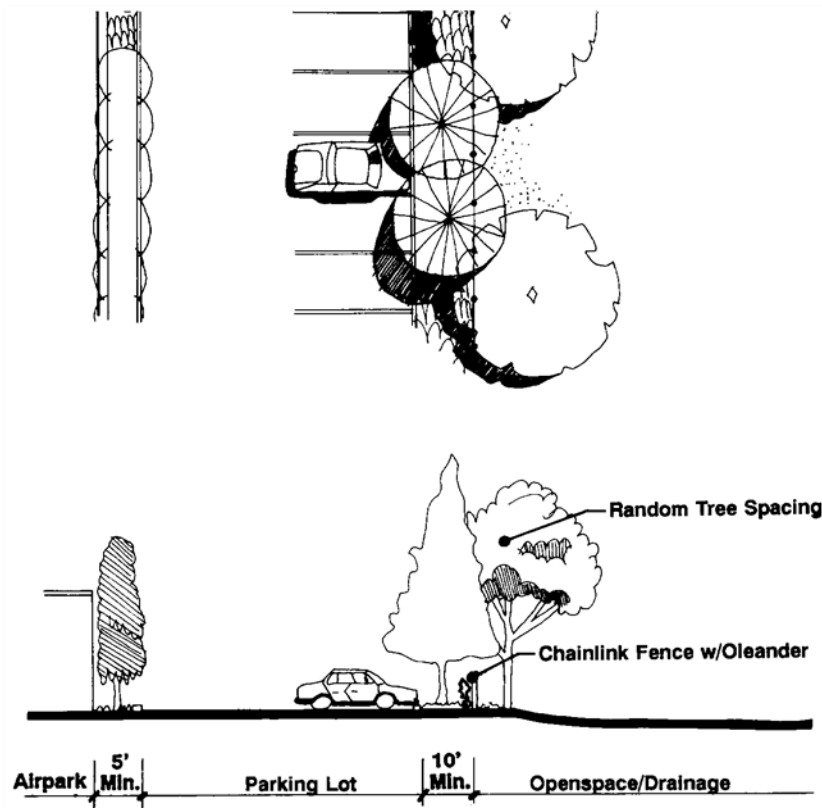
**Figure 3.4-27, Channel Buffer Detail**



**Airpark/Drainage**

The drainage channel(s) which pass through or adjacent to the airpark/mixed use development should receive special buffer treatment. A fence with oleander plantings should be provided along the rear parking lot of the airpark uses. Tree plantings with random spacing should be used to provide additional screening and to discourage unwanted use of intrusion into the drainage areas as reflected in **Figure 3.4-28, Airpark Drainage Buffer**.

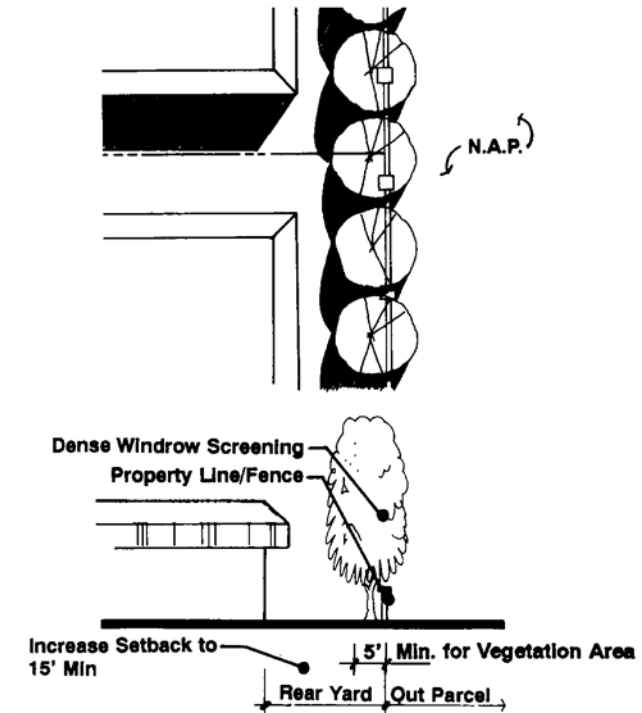
**Figure 3.4-28, Airpark Drainage Buffer**



**Out Parcel/Adjacent Properties**

Where proposed land uses are located adjacent to properties that are "Not a Part" of the Kohl Ranch Specific Plan area, buffers are recommended. Dense windrow plantings along the rear yard property line, along with fencing and an increased rear yard setback of 15 feet, should serve to minimize potential conflicts as reflected in **Figure 3.4-29, Out Parcel/Adjacent Property Buffer**.

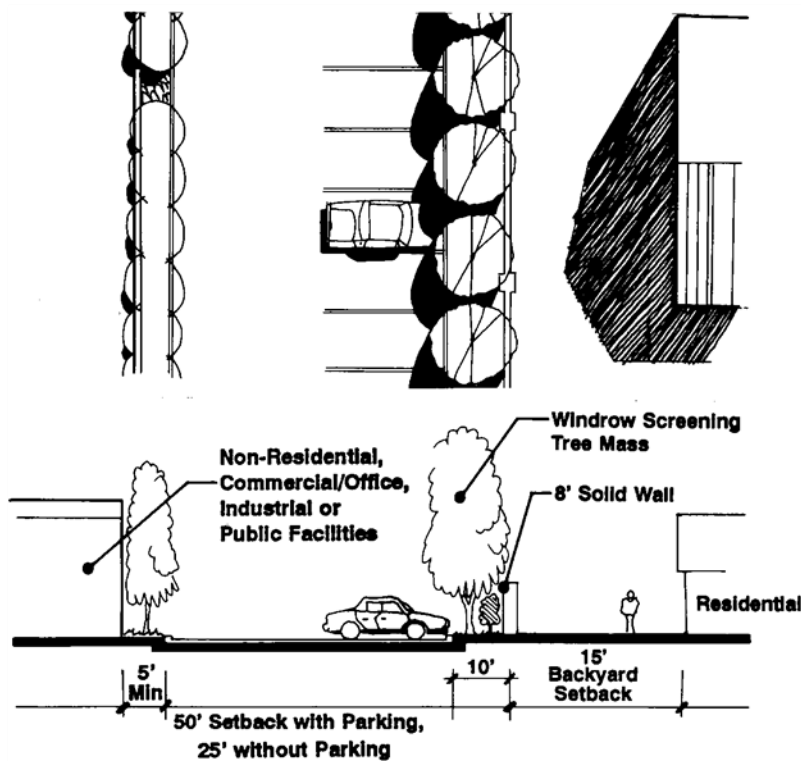
**Figure 3.4-29, Out Parcel/Adjacent Property Buffer**



**Residential/Non-Residential**

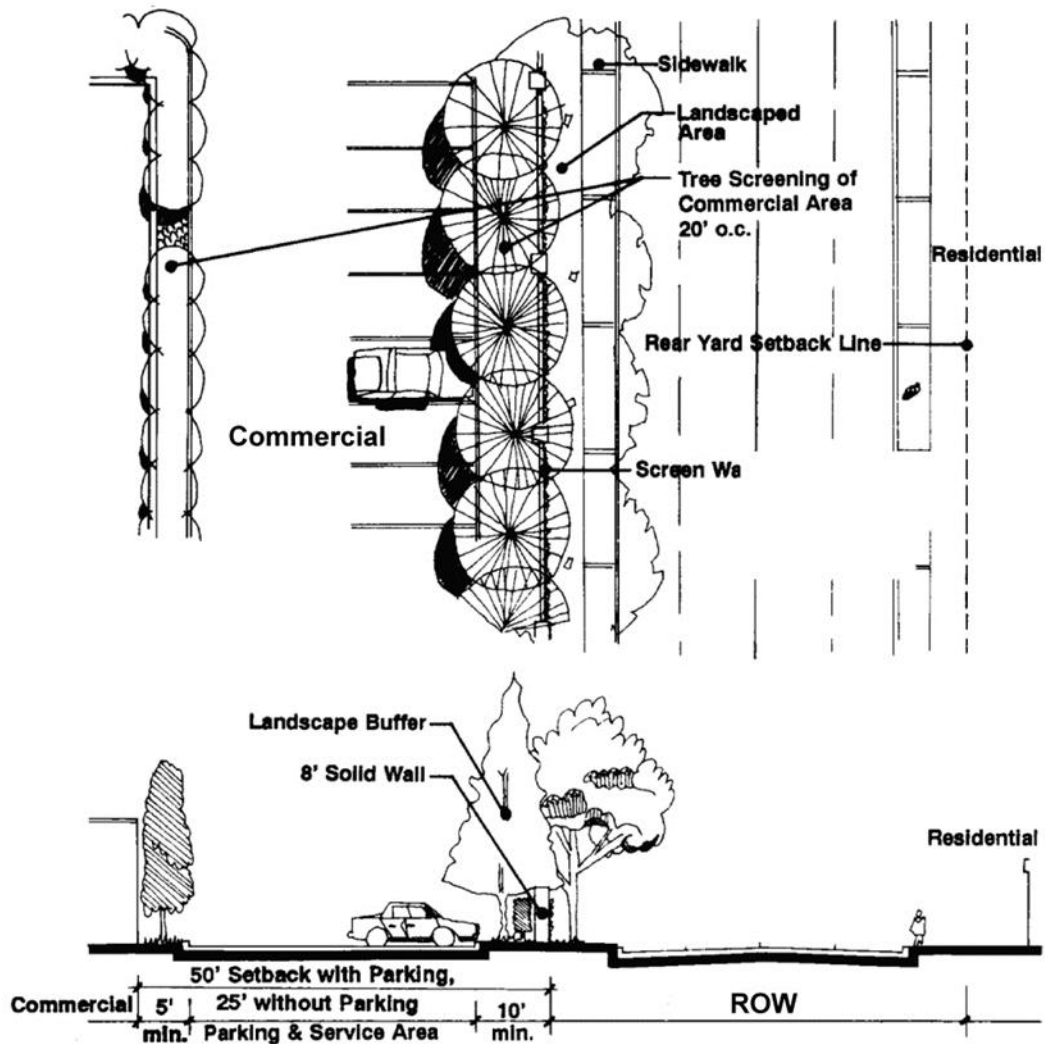
As part of the town center concept, residential uses are located near commercial areas to minimize vehicle trips and instill a sense of community. At the interface between residential and commercial land uses, buffers are needed to minimize potential adverse effects. As indicated in **Figure 3.4-30, Land Use Buffer**, a 15-foot backyard setback for the residential uses is recommended. In addition, a windrow screening tree mass should be planted along the rear property line, along an 8-foot solid wall, creating a 10-foot wide buffer area. A minimum 50-foot setback for the commercial property, inclusive of this buffer, should be maintained with parking; without parking, the buffer should be 25 feet.

**Figure 3.4-30, Land Use Buffer**



**Figure 3.4-31, Commercial Buffer Streetscape** depicts the scenario where commercial uses are located across the street from residential uses, as is the case along Tyler Street just north of Avenue 62 or on "D" Street. A landscape buffer should be planted adjacent to an 8-foot solid wall and the parking and service area for the commercial use, within the 50-foot setback. This will provide both a physical and visual separation between the commercial and residential uses.

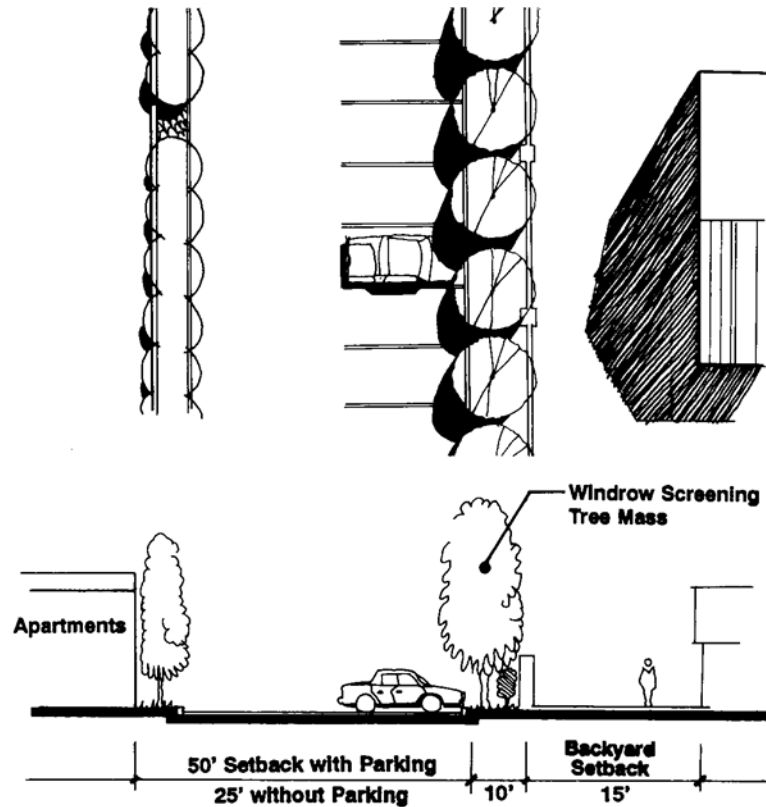
**Figure 3.4-31, Commercial Buffer Streetscape**



**Single-Family/Multi-Family Residential**

Where single-family residential dwellings are located adjacent to multi-family residential units, a product type buffer should be implemented, consisting of windrow screening along the back side of the multi-family parking area, a community wall, and an increased backyard setback for the single-family uses as reflected in **Figure 3.4-32, Product Type Buffer**.

**Figure 3.4-32, Product Type Buffer**



### 3.5 RCIP GENERAL PLAN POLICY CONSISTENCY ANALYSIS

#### 3.5.1 RCIP General Plan and Eastern Coachella Valley Area Plan Policy Consistency Analysis

**Table 3.5-A, RCIP General Plan and ECVAP Policies Consistency Analysis**, below, relates the proposed The Kohl Ranch Specific Plan No 303, Amendment 2 to relevant policies in the General Plan (“General Plan” or “RCIP”), as well as any relevant policies from the Eastern Coachella Valley Area Plan. Policies deemed not relevant to the project, based on proposed land uses, are not included in this table. Based on the brief analysis here and the overall information provided in The Kohl Ranch Specific Plan No. 303 Amendment 2, EIR 396 and Addendum (EA42375), a notation of “**C**” meaning generally consistent, or “**I**” meaning generally inconsistent, or “**N/A**” meaning not applicable, is given each policy to reflect the project’s response to the intent of the General Plan or Area Plan policy.

**Table 3.5-A, RCIP General Plan and ECVAP Policy Consistency Analysis**

Section	General Plan Policy No.	General Plan Policies	Relationship of the Project to the Policy
Aesthetics	LU 13.1	Preserve and protect outstanding scenic vistas and visual features for the enjoyment of the traveling public.	The project area is relatively flat and because of the limited development on the site, views of the nearby mountains framing the valley floor are generally unobstructed. The Santa Rosa and San Jacinto Mountains are important visual resources in the Coachella Valley. Views of these resources have been preserved in the Kohl Ranch Specific Plan Design Guidelines. <b>C</b>
Aesthetics	LU 13.2	Incorporate riding, hiking, and bicycle trails and other compatible public recreational facilities within scenic corridors.	Approximately 376 acres of open space is included in The Kohl Ranch Specific Plan. Additionally, the Project includes a trail system consisting of local and regional connections to link parks, open space and community facilities. <b>C</b>
Aesthetics	LU 13.3	Ensure that the design and appearance of new landscaping, structures, equipment, signs, or grading within Designated and Eligible State and County scenic highway corridors are compatible with the surrounding scenic setting or environment.	The Project is not located within a scenic highway or corridor. <b>N/A</b>
Aesthetics	LU 13.4	Maintain at least a 50-foot setback from the edge of the right-of-way for new development adjacent to Designated and Eligible State and County Scenic Highways.	The proposed Project is not located within a scenic highway or corridor. The closest State-eligible scenic highway is State Route 111, from Bombay Beach on the Salton Sea to State Route 195 near Mecca located approximately 4.4 miles to the southeast. <b>N/A</b>
Aesthetics	LU 13.5	Require new or relocated electric or communication distribution lines, which would be visible from Designated and Eligible State and County Scenic Highways, to be placed underground.	The Project is not located within a scenic highway or corridor. <b>N/A</b>
Aesthetics	LU 13.6	Prohibit offsite outdoor advertising displays that are visible from Designated and Eligible State and County Scenic Highways.	The Project is not located within a scenic highway or corridor. <b>N/A</b>
Aesthetics	LU 13.7	Require that the size, height, and type of on-premise signs visible from Designated and Eligible State and County Scenic Highways be the minimum necessary for identification. The design, materials, color, and location of the signs shall blend with the environment, utilizing natural materials where possible.	The Project is not located within a scenic highway or corridor. <b>N/A</b>
Aesthetics	LU 13.8	Avoid the blocking of public views by solid walls.	The Design Guidelines of the proposed project requires the use of view fencing along open space, golf courses, parks and schools to avoid blocking of public views. <b>C</b>
Aesthetics	ECVAP 4.1	Require the inclusion of outdoor lighting features that would minimize the effects on the nighttime sky and wildlife habitat areas.	The Design Guidelines of the proposed project require lighting to be designed to minimize sky glow and the nighttime desert sky. The aesthetics section of this EIR evaluated potential impacts and determined that all potential significant adverse impacts associated with the proposed project were

Section	General Plan Policy No.	General Plan Policies	Relationship of the Project to the Policy
			found to be less than significant without mitigation. In addition, conditions recommended by the Airport Land use Commission also limit lighting within the area due to its proximity to the airport so the effects of nighttime lighting will be minimized. <b>C</b>
Aesthetics	ECVAP 4.2	Adhere to the County's lighting requirements for standards that are intended to limit light leakage and spillage that may interfere with the operations of the Palomar Observatory.	The Kohl Ranch Specific Plan is located within Zone B of the Mt. Palomar Nighttime Lighting Policy Area. Mitigation measures herein reduce potential significant impacts to less than significant. Future development proposals will be required to comply with County Ordinance No. 655 and will be reviewed at such time as development application is files with the Planning Department. <b>C</b>
Aesthetics	ECVAP 10.1	Adhere to the Advertising Regulations of the County Land Use Ordinance, Section 19, regarding outdoor advertising for all development within the Eastern Coachella Valley.	Section 19 does not allow outdoor advertising within land zoned Specific Plan (SP). <b>C</b>
Aesthetics	ECVAP 10.2	Prohibit the placement of billboards within the Eastern Coachella Valley.	The zoning ordinance prohibits outdoor advertising displays. <b>C</b>
Aesthetics	ECVAP 10.3a	For premises adjacent to the right-of-way of scenic corridors, single support free-standing signs for on-site advertising shall be prohibited. A sign affixed to buildings, a free-standing monument sign, or a free-standing sheathed-support sign which has minimal impact on the scenic setting shall be utilized for on-site advertising purposes along the below-referenced scenic corridors. a. For purposes of this policy, scenic corridors include: • State Route 86; and • State Route 111.	The Project is not located within a scenic highway or corridor. <b>N/A</b>
Aesthetics	ECVAP 10.3c	On-site advertising signs for businesses located along freeway scenic corridors shall comply with the following:(1) Businesses located within 660 feet of the terminus of a freeway exit ramp or the origination of a freeway entrance ramp may utilize either monument or sheathed-support signs in addition to signs affixed to buildings.i. A free-standing monument sign for a single business or tenant may be approved with a maximum height of 10 feet and a maximum surface area of 150 square feet. A free-standing monument sign for multiple businesses or tenants may be approved with an overall height of 12 feet or less and a maximum surface area of 200 square feet.ii. A free-standing sheathed-support sign for a single business or tenant may be approved with a maximum height of 35 feet and a maximum surface area of 150 square feet. A free-standing sheathed-support sign for multiple businesses or tenants may be approved with a maximum height of 35 feet. The maximum surface area shall be the greater of either 150 square feet, or .25 percent (1/4 of 1 percent) of the total existing building floor area, except that in any event, no sign shall exceed 200 square feet in surface area.iii. A sign affixed to a building, advertising the business contained therein, shall not exceed 10 percent of the surface area of the building wall facing the freeway. A single sign, or a total of all signs, affixed to a building and advertising multiple businesses contained therein shall not exceed 10 percent of the surface area of the building wall facing the freeway.(2) Businesses located within 330 feet of the nearest edge of a freeway right-of-way line, but farther than 660 feet from the terminus of a freeway exit ramp or the origination of a freeway entrance ramp, may utilize either monument or sheathed-support signs in addition to signs affixed to buildings.i. A free-standing monument sign for a single business or tenant may be approved with a maximum height of 10 feet and a maximum surface area of 150 square feet. A free-standing monument sign for multiple businesses or tenants may be approved with an overall height of 12 feet or less and a maximum surface area of 200 square feet.ii. A free-standing sheathed-support sign for a single business or tenant may be approved with a maximum height of 25 feet, or the actual height of the primary building advertised, whichever is less, and a maximum surface area of 150 square feet. A free-standing sheathed-support sign for multiple businesses or tenants may be approved with an overall height of 25 feet, or the actual height of the primary building	The Project is not located within a scenic highway or corridor. <b>N/A</b>



Section	General Plan Policy No.	General Plan Policies	Relationship of the Project to the Policy
		advertised, whichever is less. The maximum surface area shall be the greater of either 150 square feet, or .25 percent (1/4 of 1 percent) of the total existing building floor area, except that in any event, no sign shall exceed 200 square feet in surface area.iii. A sign affixed to a building, advertising a single business contained therein, shall not exceed 10 percent of the surface area of the building wall facing the freeway. A single sign, or a total of all signs, affixed to a building and advertising multiple businesses contained therein shall not exceed 10 percent of the surface area of the building wall facing the freeway.	
Aesthetics	ECVAP 10.3d	On-site advertising signs for businesses located along highway scenic corridors shall comply with the following:(1) Notwithstanding the other provisions of this policy, a single-business monument sign may be approved with a maximum height of 10 feet, and a maximum 150 square feet of sign surface area. A multiple-business monument sign may be approved with a maximum height of 12 feet or less, and a maximum 200 square feet of sign surface area.(2) Notwithstanding the other provisions of this policy, a single-business sheathed-support sign or a multiple-business sheathed-support sign shall not be erected along a highway scenic corridor.(3) A sign affixed to a building, advertising the business contained therein, shall not exceed 10 percent of the surface area of the building wall facing the highway. A single sign, or a total of all signs, affixed to a building and advertising multiple businesses contained therein shall not exceed 10 percent of the surface area of the building wall facing the highway.	The Project is not located within a scenic highway or corridor. <b>N/A</b>
Aesthetics	ECVAP 14.1	Protect the scenic highways in the Eastern Coachella Valley from change that would diminish the aesthetic value of adjacent properties in accordance with the Scenic Corridors section of the General Plan Land Use, Multipurpose Open Space, and Circulation Elements.	The Project is not located within a scenic highway or corridor. <b>N/A</b>
Aesthetics	ECVAP 16.1	Refer to the Ridgeline policies in the Hillside Development and Slope section of the General Plan Land Use Element and the Scenic Resources policies in the General Plan Multipurpose Open Space Element.	The ridgeline of the Santa Rosa mountains along the western edge of the Eastern Coachella Valley is an important natural resource. The project area is relatively flat and because of the limited development on the site and requirements in the Kohl Ranch Specific Plan Design Guidelines, views of the nearby mountains framing the valley floor are generally unobstructed. <b>C</b>
Aesthetics	OS 21.1	Identify and conserve the skylines, view corridors, and outstanding scenic vistas within Riverside County.	The project area is relatively flat and because of the limited development on the site, views of the nearby mountains framing the valley floor are generally unobstructed. The Santa Rosa and San Jacinto Mountains are important visual resources in the Coachella Valley. Views of these resources have been preserved in the Kohl Ranch Specific Plan Design Guidelines. <b>C</b>
Aesthetics	OS 22.1	Design developments within designated scenic highway corridors to balance objectives of maintaining scenic resources with accommodating compatible land uses.	The Project is not located within a scenic highway or corridor. <b>N/A</b>
Aesthetics	OS 22.3	Encourage joint efforts among federal, state, and County agencies, and citizen groups to ensure compatible development with scenic corridors.	The Project is not located within a scenic highway or corridor. <b>N/A</b>
Aesthetics	OS 22.4	Impose conditions on development within scenic highway corridors requiring dedication of scenic easements consistent with the Scenic Highways Plan, when it is necessary to preserve unique or special visual features.	The Project is not located within a scenic highway or corridor. <b>N/A</b>
Aesthetics	OS 22.5	Utilize contour grading and slope rounding to gradually transition graded road slopes into a natural configuration consistent with the topography of the areas within scenic highway corridors.	The Project is not located within a scenic highway or corridor. <b>N/A</b>
Agriculture	ECVAP 5.1	Retain and protect agricultural lands through adherence to the policies contained in the Agriculture section of the General Plan Land Use Element.	Section V.C.2 of the EIR incorporates mitigation requiring adherence to Ordinance 625 to protect agricultural uses. To avoid potential impact, a 300-foot setback between development and offensive

Section	General Plan Policy No.	General Plan Policies	Relationship of the Project to the Policy
			agricultural uses is required as mitigation. The Kohl Ranch Specific Plan does not include any agriculturally designated land and no changes are proposed that would affect other areas of agricultural use. <b>N/A</b>
Agriculture	ECVAP 5.2	Refer to the General Plan Certainty System in the General Plan Administrative Element. An exception is provided allowing limited changes from the Agriculture designation to be processed and approved.	There are no agriculturally designated lands in the Kohl Ranch Specific Plan Amendment No. 2. <b>N/A</b>
Agriculture	ECVAP 6.1	Allow farmworker housing that meets basic safety standards in agriculturally designated areas per the land use designations section of the General Plan Land Use Element, and the Five-Year Action Plan and Special Housing Need sections of the Housing Element. Provided that adequate provisions for public services and compatibility with adjacent uses is achieved, farm worker housing projects of both 1-12 dwelling units and greater than 12 units are permitted in the Agriculture designation in the Eastern Coachella Valley Area Plan.	There are no agriculturally designated lands in the Kohl Ranch Specific Plan Amendment No. 2. <b>N/A</b>
Agriculture	LU 16.1	Encourage retaining agriculturally designated lands where agricultural activity can be sustained at an operational scale, where it accommodates lifestyle choice, and in locations where impacts to and from potentially incompatible uses, such as residential uses, are minimized, through incentives such as tax credits.	The Eastern Coachella Valley Area Plan contains 41,403 acres of AG designated land which is 9.1 percent of all the land comprising this Area Plan. Within Riverside County, only the San Jacinto Valley Area Plan designates a greater percentage of land to AG with 9.4 percent. The Kohl Ranch Specific Plan would not retain the Agriculture (AG) designated lands within its project area. Section V.C.2 of the EIR analyzed impacts and determined these to be significant and unavoidable. A statement of overriding considerations was adopted with certification of the EIR November 16, 1999. A General Plan Amendment was approved to change land use to that of an adopted specific plan and a zone change changed the zoning to Specific Plan (SP). This policy is therefore, not applicable to the Kohl Ranch Specific Plan Amendment No. 2 with the exception of minimizing impacts to agriculturally designated lands from incompatible uses. EIR section V.C.2 incorporates mitigation requiring adherence to Ordinance 625 to protect agricultural uses. To avoid potential impact, a 300-foot setback between development and offensive agricultural uses is required as mitigation. <b>N/A, C</b>
Agriculture	LU 16.2	Protect agricultural uses, including those with industrial characteristics (dairies, poultry, hog farms, etc) by discouraging inappropriate land division in the immediate proximity and allowing only land uses and intensities that are compatible with agricultural uses.	Section V.C.2 of the EIR incorporates mitigation requiring adherence to Ordinance 625 to protect agricultural uses. To avoid potential impact, a 300-foot setback between development and offensive agricultural uses is required as mitigation. No such offensive agricultural uses exist in proximity to the Kohl Ranch. <b>C</b>
Air Quality	LU 10.1	Provide sufficient commercial and industrial development opportunities in order to increase local employment levels and thereby minimize long-distance commuting (AI 18)	The Land Use Designations in the area north of Avenue 62 and east of Tyler Street is intended for industrial and airport-related business park development. Additional land use designations along both sides of Avenue 62 are intended for commercial development. These uses will increase the available employment opportunities and help minimize long-distance commuting. Bus stops and coordination with the transit agency for increased routes will also provide alternatives to long-distance commutes. <b>C</b>
Air Quality	LU 10.2	Ensure adequate separation between pollution producing activities and sensitive emission receptors, such as hospitals, residences, and schools.	EIR section V.C.6 and the Air Quality section of the Addendum incorporate mitigation measures to reduce impacts to sensitive receptors. <b>C</b>
Air Quality	LU 10.3	Accommodate the development of community centers and concentrations of development to reduce reliance on the automobile and help improve air quality.	The Kohl Ranch Specific Plan creates its own master-planned community consisting of medium density, medium high density, high density, and very high density residential types, three existing schools, commercial, industrial, parks and open space that collectively may contain the attributes that contribute to a community center. EIR section V.D.1 incorporates mitigation to provide for potential bus transit as well as the opportunity for Park and Ride facilities. The project includes a trail system. This system will include a combination of greenbelts with linkages and sidewalks located adjacent to roads. It will incorporate trails for pedestrians, bicyclists, and equestrians in the form of an integrated system of hard- and soft-surface trails. The trail system will promote non-vehicular access to on-site recreational areas as well as to the schools thus reducing the reliance on automobiles. <b>C</b>

Section	General Plan Policy No.	General Plan Policies	Relationship of the Project to the Policy
Air Quality	LU 10.4	Provide options to the automobile in communities, such as transit, bicycle and pedestrian trails, to help improve air quality.	EIR section V.D.1 incorporates mitigation to provide for potential bus transit as well as the opportunity for Park and Ride facilities. The project includes a trail system. This system will include a combination of greenbelts with linkages and sidewalks located adjacent to roads. It will incorporate trails for pedestrians, bicyclists, and equestrians in the form of an integrated system of hard- and soft-surface trails. The trail system will promote non-vehicular access to on-site recreational areas as well as to the schools. <b>C</b>
Air Quality	AQ 4.7	To the greatest extent possible, require every project to mitigate any of its anticipated emissions that exceed allowable emissions as established by the SCAQMD, MDAQMD, SOCAB, the Environmental Protection Agency, and the California Air Resources Board.	The project is located within the jurisdiction of the South Coast Air Quality Management District (SCAQMD). The air quality section of the EIR requires that the project comply with the requirements set forth by the SCAQMD, the Environmental Protection Agency, and the California Air Resources Board and propose to mitigate, to the greatest extent possible, project-related emissions. <b>C</b>
Air Quality	AQ 8.2	Emphasize job creation and reductions in vehicle miles traveled in job-poor areas to improve air quality over other less efficient methods.	The proposed project includes a commercial and industrial uses only along Avenue 62 and east of Tyler Street which will create jobs and concentrate growth around the center which will help reduce vehicle miles traveled. <b>C</b>
Air Quality	AQ 8.4	Support new mixed-use land use patterns and community centers which encourage community self-sufficiency and containment, and discourage automobile dependency.	EIR section V.D.1 incorporates mitigation to provide for potential bus transit as well as the opportunity for Park and Ride facilities. The project includes a trail system. This system will include a combination of greenbelts with linkages and sidewalks located adjacent to roads. It will incorporate trails for pedestrians, bicyclists, and equestrians in the form of an integrated system of hard- and soft-surface trails. The trail system will promote non-vehicular access to on-site recreational areas as well as to the schools. <b>C</b>
Biology	ECVAP 15.1	Protect visual and biological resources in the Eastern Coachella Valley Area Plan through adherence to General Plan policies found in the Fish and Wildlife Habitat section of the Multipurpose Open Space Element, as well as policies contained in the Coachella Valley Multiple Species Habitat Conservation Plan, upon its adoption.	The proposed Project site has been extensively disturbed from agricultural activities. Section V.C.3 of this EIR analyzed potential impacts to biological resources. The EIR incorporates a mitigation measure within the Biology section, which requires a Pre-Construction Survey for burrowing owls. No other impacts were found to be significant. Additionally, the Addendum incorporates a mitigation measure to account for the Coachella Valley Multiple Species Habitat Conservation Plan requiring the payment of fees prior to grading. <b>C</b>
Cultural	OS 19.2	Review all proposed development for the possibility of archaeological sensitivity.	Section V.C.12 of this EIR analyzed potential impacts to archeological resources. The EIR incorporates mitigation measures within the cultural resources section that reduce impacts to archeological sites and resources. <b>C</b>
Cultural	OS 19.3	Employ procedures to protect the confidentiality and prevent inappropriate public exposure of sensitive archaeological resources when soliciting the assistance of public and volunteer organizations.	Section V.C.12 of this EIR analyzed potential impacts to archeological resources. The EIR incorporates mitigation measures within the cultural resources section that reduce impacts to archeological sites and resources. No confidential appendices to the archaeological technical reports have been released to the public. <b>C</b>
Cultural	OS 19.4	Require a native American Statement as part of the environmental review process on development projects with identified cultural resources.	The Kohl Ranch Specific Plan Amendment No. 2 is required to comply with Senate Bill 18. <b>C</b>
Cultural	OS 19.5	Transmit significant development proposals to the History Division of the Riverside County Regional Park and Open-Space District for evaluation in relation to the destruction/preservation of potential historical sites. Prior to approval of any development proposal, feasible mitigation shall be incorporated into the design of the project and its conditions of approval.	EIR section V.C.12 and the Cultural Resources section of the Addendum incorporate mitigation measures to reduce impacts to historical sites and resources including discovered resources. The proposed project has been reviewed by the Regional Park and Open Space District. <b>C</b>
Cultural	OS 19.6	Enforce the Historic Building Code so that historical buildings can be preserved and used without posing a hazard to public safety.	Historic buildings were not identified on site. <b>N/A</b>

Section	General Plan Policy No.	General Plan Policies	Relationship of the Project to the Policy
Cultural	OS 19.7	When possible, allocate resources and/or tax credits to prioritize retrofit of County historic structures, which are irreplaceable.	Historic buildings were not identified on site. <b>N/A</b>
Cultural	OS 19.8	Whenever existing information indicates that a site proposed for development may contain biological, paleontological, or other scientific resources, a report shall be filed stating the extent and potential significance of the resources that may exist within the proposed development and appropriate measures through which the impacts of development may be mitigated.	Cultural and paleontological assessments have been performed for this project and are discussed in Section V.C.12 of this EIR which includes mitigation measures to lessen potential impacts. <b>C</b>
Cultural	OS 19.9	This policy requires that when existing information indicates that a site proposed for development may contain paleontological resources, a paleontologist shall monitor site grading activities, with the authority to halt grading to collect uncovered paleontological resources, curate any resources collected with an appropriate repository, and file a report with the Planning Department documenting any paleontological resources that are found during the course of site grading.	Section V.C.12 of the EIR includes mitigation measures which call for paleontological monitoring within areas identified as potentially significant and includes measures should inadvertent discoveries be made. <b>C</b>
Cultural	OS 19.10	Transmit significant development applications subject to CEQA to the San Bernardino County Museum for review, comment, and/or preparation of recommended conditions of approval with regard to paleontological resources.	Section V.C.12 of the EIR includes mitigation measures which call for paleontological monitoring within areas identified as potentially significant and includes measures should inadvertent discoveries be made. <b>C</b>
Geology & Soils	ECVAP 18.1	Protect life and property from wildfire hazards through adherence to the Fire Hazards section of the General Plan Safety Element	The proposed Project is located in an area considered to be at very low susceptibility for wildfire. <b>N/A</b>
Geology & Soils	ECVAP 19.1	Protect life and property from seismic-related incidents through adherence to the Seismic Hazards section of the General Plan Safety Elements.	Section V.C.4 of the EIR analyzed potential impacts from seismic-related incidents and the proposed Project site has the potential to be subject to strong seismic ground shaking events. The EIR incorporates mitigation measures within the geology and seismicity section to reduce impacts by requiring that structures be constructed per the California Building Code. <b>C</b>
Geology & Soils	ECVAP 20.1	Protect life and property through adherence to the Hillside Development and Slope section of the General Plan Land Use Element and the Slope and Soil Instability Hazards section of the General Plan Safety Element.	The project is not in an area of steep slope. Section V.C.1 of the EIR analyzed potential impacts from slope and instability hazards. The EIR incorporates mitigation measures within the land form and topography section to reduce impacts by requiring that grading activities be in conformance with the California Building Code and Riverside County Ordinance 457. <b>C</b>
Geology & Soils	ECVAP 20.2	Refer to the Rural Mountainous and Open Space-Rural land use designations in the General Plan Land Use Element.	The project site does not contain these land use designations. <b>N/A</b>
Geology & Soils	ECVAP 21.1	Minimize damage from and exposure to wind erosion and blowsand through adherence to the Slope and Soil Instability Hazards section of the General Plan Safety Element.	Implementing projects within the proposed Specific Plan will be required to comply with Riverside County Ordinance 742. <b>C</b>
Geology & Soils	ECVAP 21.2	Require protection of soil in areas subject to wind erosion or blowsand. Mitigation measures that may be required include, but are not limited to, windbreaks, walls, fences, vegetative groundcover, rock, other stabilizing materials, and installation of an irrigation system or provision of other means of irrigation.	Implementing projects within the proposed Specific Plan will be required to comply with Riverside County Ordinance 742. Additionally, the Specific Plan landscape guidelines incorporate windrow plantings to provide wind breaks. <b>C</b>
Geology & Soils	ECVAP 21.3	Control dust through the policies of the Particulate Matter section of the General Plan Air Quality Element.	EIR section V.C.6 incorporates mitigation to control fugitive dust and particulate matter. <b>C</b>
Geology & Soils	ECVAP 21.4	Preserve the environmentally sensitive alluvial fan areas flowing out of the canyons of the Santa Rosa Mountains.	EIR sections V.C.1 and V.C.12 incorporate mitigation measures to ensure protection of alluvial fan areas. <b>C</b>

Section	General Plan Policy No.	General Plan Policies	Relationship of the Project to the Policy
Geology & Soils	S 2.5	Require that engineered slopes be designed to resist seismically induced failure. For lower-risk projects, slope design could be based on pseudo-static stability analyses using soil engineering parameters that are established on a site-specific basis. For higher-risk projects, the stability analyses should factor in the intensity of expected ground shaking, using a Newmark-type deformation analysis.	This project will comply with Ordinance 457, which requires that manufactured slopes are designed to be seismically safe. Additionally, Section V.C.1 of the EIR includes mitigation measures which call for grading activities to conform with the California Building Code and Ordinance 457. <b>C</b>
Geology & Soils	S 3.5	During permit review, identify and encourage mitigation of on-site and off-site slope instability, debris flow, and erosion hazards on lots undergoing substantial improvements.	During the review of implementing applications (residential subdivisions, use permits, etc.) of The Kohl Ranch Specific Plan, these geological issues will be analyzed and addressed on an as-needed basis. <b>C</b>
Geology & Soils	LU 11.1c	Require that areas with slope be developed in a manner to minimize the hazards from erosion and slope failures.	The project area is relatively flat and is not in an area of steep slope. Sections V.C.1 and V.C.5 of the EIR include mitigation measures for erosion control. <b>C</b>
Geology & Soils	LU 11.1f	Encourage the limitation of grading, cut, and fill to the amount necessary to provide stable areas for structural foundations, street right of way, parking facilities, and other intended uses.	The Kohl Ranch has been designed to limit as much as possible the amount of grading in the project area. <b>C</b>
Geology & Soils	S 7.7b	Require mitigation measures to reduce potential damage caused by ground failure for sites determined to have potential for liquefaction. Such measures shall apply to critical facilities, utilities, and large commercial and industrial projects as a condition of project approval.	The proposed project is designated as having a high potential for liquefaction. A Geotechnical Report prepared for the project site found the potential to be low, however, and sections V.C.4 of the EIR includes mitigations measures for further site specific investigations once location and nature of structures are known. Additionally, conformance with California Building Code standards, enforcement of the maintenance of the tile drain system, seismic safety standards and sound grading practices will help to mitigate any potentially concerning circumstances. <b>C</b>
Geology & Soils	S 4.1	For new construction and proposals for substantial improvements to residential and nonresidential development with 100-year floodplains as mapped by FEMA or as determined by site specific hydrologic studies for areas not mapped by FEMA, the County shall apply a minimum level of acceptable risk; and disapprove projects that cannot mitigate the hazard to the satisfaction of the Building Official or other responsible agency.	Implementing projects within the proposed Specific Plan will comply with the Flood and Inundation section of the General Plan Safety Element. <b>C</b>
Hazards	ECVAP 18.1	Protect life and property from wildfire hazards through adherence to the Fire Hazards section of the General Plan Safety Element	The Kohl Ranch is located in an area considered to be at a very low susceptibility for wildfire. <b>C</b>
Hazards	S 6.1.a.	Comply with federal and state laws pertaining to the management of hazardous wastes and materials.	The project will be in compliance with existing regulations such as South Coast Air Quality Management District Rules and Regulations pertaining to asbestos, Department of Transportation (DOT) office of Hazardous Materials Safety regulations, and Titles 8, 22, and 26 of the California Code of Regulations, would ensure that the public would not be exposed to any unusual or excessive risks related to hazardous materials. <b>C</b>
Hazards	S 7.1	Continually strengthen the Multi-Hazard Functional Plan and maintain mutual aid agreements with federal, state, local agencies and the private sector to assist in: a. clearance of debris in the event of widespread slope failures, collapsed buildings or structures, or other circumstances that could result in blocking emergency access or regress; b. heavy search and rescue; c. fire suppression; d. hazardous materials response; e. temporary shelter; f. geologic and engineering needs; g. traffic and crowd control; and h. building inspection.	The project site along with the unincorporated Riverside County areas are contracted with the Riverside County Fire Department for emergency response. Emergency response and emergency evacuation are regulated under one agency in the project area and surrounding areas. Therefore project development will not interfere with existing emergency response and evacuation, but will be consistent with the existing system because the same agency regulates all of the surrounding areas. <b>C</b>
Hydrology	ECVAP 17.1	Protect life and property from the hazards of flood events through adherence to the Flood and Inundation Hazards section of the General Plan Safety Element.	The proposed The Kohl Ranch Specific Plan is not within the 100-year flood plain but the area is subject to ponding and flash flooding. EIR sections V.C.5 and V.D.2 incorporate mitigation measures to mitigate against impacts from flood damage, surface ponding and erosion. <b>C</b>
Hydrology	ECVAP 17.2	Adhere to the flood proofing, flood protection requirements, and Flood Management Review requirements of the Riverside County Ordinance No. 458 Regulating Flood Hazard	The proposed The Kohl Ranch Specific Plan is not within the 100-year flood plain but the area is subject to ponding and flash flooding. EIR sections V.C.5 and V.D.2 incorporate mitigation measures to

Section	General Plan Policy No.	General Plan Policies	Relationship of the Project to the Policy
		Areas.	mitigate against impacts from flood damage, surface ponding and erosion. Additionally, the proposed project will comply with Ordinance 458. <b>C</b>
Hydrology	ECVAP 17.3	Require that proposed development projects that are subject to flood hazards, surface ponding, high erosion potential or sheet flow be submitted to the Coachella Valley Water District for review.	The proposed The Kohl Ranch Specific Plan is not within the 100-year flood plain but the area is subject to ponding and flash flooding. EIR sections V.C.5 and V.D.2 incorporate mitigation measures to mitigate against impacts from flood damage, surface ponding and erosion. <b>C</b>
Hydrology	OS 3.3	Minimize pollutant discharge into storm drainage systems and natural drainage and aquifers.	The Hydrology section of this EIR evaluated potential impacts to water quality and determined that all potential significant adverse impacts associated with the proposed project were found to be less than significant. With the implementation of the Regional WQMP, as well as subsequent tract specific WQMPs that will be prepared at the time of tract map submittal, impacts to water quality are anticipated to be less than significant. <b>C</b>
Hydrology	OS 5.3	Based on site specific study, all development shall be set back from the floodway boundary a distance adequate to address the following issues: a. public safety; b. erosion; c. riparian or wetland buffer; d. wildlife movement corridor or linkage; and e. slopes.	Implementing projects within the proposed Specific Plan will comply with the Flood and Inundation section of the General Plan Safety Element. Upon completion, the entirety of development of the Kohl Ranch is outside of any Flood Hazard areas. <b>C</b>
Hydrology	OS 5.4	Consider designating floodway setbacks for greenways, trails, and recreation opportunities on a case-by-case basis.	The project is not located within a floodway but is subject to ponding and flash flooding. EIR section V.C.5 and V.D.2 incorporate mitigation measures to reduce impacts. <b>C</b>
Hydrology	S 4.8	Allow development within the floodway fringe, if the proposed structures can be adequately flood-proofed and will not contribute to property damage or risks to public safety.	The project is not located within a floodway but is subject to ponding and flash flooding. EIR section V.C.5 and V.D.2 incorporate mitigation measures to reduce impacts. <b>C</b>
Hydrology	S 4.9	Within the floodway fringe of a floodplain as mapped by FEMA or as determined by site specific hydrologic studies for areas not mapped by FEMA, require development to be capable of withstanding flooding and to minimize use of fill. However, some development may be compatible within flood plains and floodways, as may some other land uses. In such cases, flood proofing would not be required. Compatible uses shall not, however, obstruct flows or adversely affect upstream or downstream properties with increased velocities, erosion backwater effects, or concentrations of flows.	The project is not located within a floodway but is subject to ponding and flash flooding. EIR section V.C.5 and V.D.2 incorporate mitigation measures to reduce impacts. <b>C</b>
Hydrology	S 4.18	Require that the design and upgrade of street storm drains be based on the depth of inundation, relative risk to public health and safety, the potential for hinderance of emergency access and regress from excessive flood depth, and the threat of contamination within the top of curbs and the 100-year flood flows within the street right of way.	As shown in the Drainage Plan portion of the Specific Plan, the Kohl Ranch will utilize streets, underground storm drains, swales, drainage pipes, collection basins, and catch basins to collect the on-site and off-site storm water, and convey it through the project and discharge over wiers on the east side of the project site. Facilities will be required to accommodate developed 100-year storm runoff through the project. The backbone drainage plan facilities are designed to protect habitable dwelling units from flooding. <b>C</b>
Land Use & Planning	ECVAP 3.1	To provide for the orderly development of Desert Resorts Regional Airport and Chiriaco Summit Airport and the surrounding area, comply with the Airport Land Use Compatibility Plan for Desert Resorts Regional Airport and Chiriaco Summit Airport as fully set forth in Appendix L and as summarized in Table 4, as well as any applicable policies related to airports in the Land Use, Circulation, Safety and Noise Elements of the Riverside County General Plan.	The Airport Land Use Commission has found the Kohl Ranch Specific Plan to be conditionally consistent with the 2005 Jacqueline Cochran Regional Airport (formerly Desert Resorts Regional Airport) Land Use Compatibility Plan (JCRAUCP). EIR Section V.D.12 analyzed and incorporates mitigation measures to ensure compliance with the JCRAUCP. The Kohl Ranch is not located within the policy area of the Chiriaco Summit Airport. <b>C</b>
Land Use & Planning	ECVAP 7.1	Ensure proper service provision, land use compatibility, design standards, safety, and accessibility for recreational vehicle development in the Eastern Coachella Valley area through adherence to General Plan policies found in the Land Use Element.	Resort Recreational Vehicle developments are not envisioned within the Kohl Ranch Specific Plan Amendment 2 area, therefore such policies do not apply to the project. <b>N/A</b>
Land Use & Planning	ECVAP 7.2	Allow Resort Recreational Vehicle developments within the following land use designations: Low Density Residential, Medium Density Residential, Medium High Density Residential, High	Resort Recreational Vehicle developments are not envisioned within the Kohl Ranch Specific Plan Amendment 2 area, therefore such policies do not apply to the project. <b>N/A</b>

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		Density Residential, Very High Density Residential, Commercial Tourist, and Open Space-Recreation.	
Land Use & Planning	ECVAP 7.3	Limit Resort Recreational Vehicle developments to a density of sixteen (16) spaces per acre.	Resort Recreational Vehicle developments are not envisioned within the Kohl Ranch Specific Plan Amendment 2 area, therefore such policies do not apply to the project. <b>N/A</b>
Land Use & Planning	ECVAP 7.4	Allow Remote recreational vehicle developments within the following land use designations: Very Low Density Residential, Estate Density Residential, Rural Residential, Rural Mountainous, Rural Desert, Open Space-Recreation, and Open Space-Rural.	Resort Recreational Vehicle developments are not envisioned within the Kohl Ranch Specific Plan Amendment 2 area, therefore such policies do not apply to the project. <b>N/A</b>
Land Use & Planning	ECVAP 7.5	Limit Remote recreational vehicle developments to a density of seven (7) spaces per acre.	Resort Recreational Vehicle developments are not envisioned within the Kohl Ranch Specific Plan Amendment 2 area, therefore such policies do not apply to the project. <b>N/A</b>
Land Use & Planning	ECVAP 8.1	Encourage industrial uses related to agriculture to continue and expand within this area plan.	The zoning ordinance allows for light agriculture on industrial land use designations where lots are 20 acres or greater. <b>C</b>
Land Use & Planning	ECVAP 8.2	Discourage industrial uses that may conflict with agricultural or residential land uses either directly or indirectly within the Eastern Coachella Valley Area Plan.	Section V.C.2 of the EIR incorporates mitigation requiring adherence to Ordinance 625 to protect agricultural uses. To avoid potential impact, a 300-foot setback between development and offensive agricultural uses is required as mitigation. As a planned community, potential industrial uses are planned nearest the airport and appropriate design features, such as roads and landscaping, are proposed to buffer between proposed residential land uses and industrial uses. <b>C</b>
Land Use & Planning	ECVAP 8.3	Discourage industrial uses which use large quantities of water in manufacturing or cooling processes and result in subsequent effluent discharges.	EIR sections V.D.2 incorporate mitigation measures to mitigate against impacts from usage of large quantities or subsequent effluent discharges. <b>C</b>
Land Use & Planning	ECVAP 8.4	Discourage industrial uses which produce significant quantities of toxic emissions into the air.	The project is located within the jurisdiction of the South Coast Air Quality Management District (SCAQMD). The air quality section of the EIR requires that the project comply with the requirements set forth by the SCAQMD, the Environmental Protection Agency, and the California Air Resources Board and propose to mitigate, to the greatest extent possible, project-related emissions. <b>C</b>
Land Use & Planning	ECVAP 8.5	Encourage industrial uses that can best utilize the transportation facilities of the Desert Resorts Regional Airport.	All industrial uses are located within the vicinity of the Jacqueline Cochran Regional Airport (formerly Desert Resorts Regional Airport) within the Kohl Ranch Specific Plan Land Use Plan. <b>C</b>
Land Use & Planning	ECVAP 8.6	Encourage industrial uses related to aviation to locate in the vicinity of the Desert Resorts Regional Airport.	All industrial uses are located within the vicinity of the Jacqueline Cochran Regional Airport (formerly Desert Resorts Regional Airport) within the Kohl Ranch Specific Plan Land Use Plan. <b>C</b>
Land Use & Planning	LU 2.1	Accommodate land use development in accordance with the patterns and distribution of use and density depicted on the General Plan Land Use Map (RCIP Figure VI-1) and the Area Plan Land Use Maps, in accordance with the following:	Kohl Ranch Specific Plan Amendment No. 2 proposes a reallocation of land uses previously approved in the Kohl Ranch Specific Plan. The proposed uses will be the same intensity as the uses permitted under the current Kohl Ranch Specific Plan Amendment No. 1. <b>C</b>
Land Use & Planning	LU 2.1a	Provide a land use mix at the countywide and area plan levels based on projected need and supported by evaluation of impacts to the environment, economy, infrastructure, and services.	The population of Riverside County is expected to be approximately 3.4 million by 2030 creating an increasing need for a broader variety of job and services available to the residents of the area. More housing units will be needed to accommodate new residents. At the countywide level, the RCIP Vision Statement sets forth various topical visions aimed at proactively dealing with the aforementioned need for growth with its social and development issues. By offering a broader mix of land use as proposed in the Kohl Ranch Specific Plan, the community can be better served. The proposed project provides a mix of land uses within a master planned community setting including a range of residential types, open space, public facilities, commercial and industrial use. This comprehensive approach will assist the County in meeting its vision and accommodating project growth. The EIR evaluated in detail impacts to the environment. Infrastructure necessary to serve the project area is being constructed and is evaluated in the specific plan; and public services will be provided as identified in the specific

Section	General Plan Policy No.	General Plan Policies	Relationship of the Project to the Policy
			plan and addressed the EIR. <b>C</b>
Land Use & Planning	LU 2.1b	Accommodate a range of community types and character, from agricultural and rural enclaves to urban and suburban communities.	The Kohl Ranch Specific Plan with its variety of land uses will accommodate a range of community types and character that will be compatible with both the County General Plan and Area Plans. <b>C</b>
Land Use & Planning	LU 2.1c	Provide for a broad range of land uses, intensities, and densities, including a range of residential, commercial, business, industry, open space, recreation, and public facilities uses.	The Kohl Ranch Specific Plan will create a master-planned community consisting of medium high density, high density, and very high density residential types, four potential K-8 schools, mixed-use commercial/residential, open space, and public and civic facilities. <b>C</b>
Land Use & Planning	LU 2.1d	Concentrate growth near community centers that provide a mixture of commercial, employment, entertainment, recreation, civic, and cultural uses.	The Eastern Coachella Valley area is a rural area of Riverside County which has limited employment, commercial and entertainment opportunities currently available to residence in the vicinity of the Project site. The Kohl Ranch Specific Plan concentrates the commercial and industrial uses along Avenue 62 to allow for the growth of a community center. <b>C</b>
Land Use & Planning	LU 2.1e	Concentrate growth near or within existing urban and suburban areas to maintain the rural and open space character of Riverside County to the greatest extent possible.	The Eastern Coachella Valley area is a rural area of Riverside County. The area around Jacqueline Cochran Airport is envisioned by the County as an area where growth should be focused. The Kohl Ranch Specific Plan is consistent with this goal for the area. <b>C</b>
Land Use & Planning	LU 2.1f	Site development to capitalize upon multi-modal transportation opportunities and promote compatible land use arrangements that reduce reliance on the automobile.	The Eastern Coachella Valley area is a rural area of Riverside County which has limited multi-modal transportation opportunities currently available to residence in the vicinity of the Project site. The Kohl Ranch development is expected to encourage the development of future bus routes to the area. The proposed Kohl Ranch Specific Plan offers a mix of residential/commercial services, job producing areas, schools and parks all of which are linked with a trail system so residents will be able to access either by bike or foot, reducing their reliance on automobiles. <b>C</b>
Land Use & Planning	LU 2.1g	Prevent inappropriate development in areas that are environmentally sensitive or subject to severe natural hazards.	The Kohl Ranch Specific Plan, being part of the Eastern Coachella Valley area, is designated as a high liquefaction potential, however a Geotechnical Report prepared for the project site as part of the analysis for the EIR found the potential to be low and section V.C.4 of the EIR includes mitigations measures for further site specific investigations once location and nature of structures are known. Additionally, conformance with California Building Code standards, enforcement of seismic safety standards and sound grading practices will help to mitigate any potentially concerning circumstances. While the Project site is not within the 100-year flood plain, the area is subject to ponding and flash flooding. EIR sections V.C.5 and V.D.2 incorporate mitigation measures to mitigate against impacts from flood damage. <b>C</b>
Land Use & Planning	LU 3.1	Accommodate land use development in accordance with the patterns and distribution of use and density depicted on the General Plan Land Use Maps (RCIP Figure VI-1) and the Area Plan Land Use Maps in accordance with the following concepts:	See LU2.1 for setting details. <b>C</b>
Land Use & Planning	LU 3.1a	Accommodate communities that provide a balanced mix of land uses, including employment, recreation, shopping and housing.	See response to Policy LU2.1, above. <b>C</b>
Land Use & Planning	LU 3.1b	Assist in and promote the development of infill and underutilized parcels which are located in Community Development areas, as identified on the General Plan Land Use Map.	The Kohl Ranch is not located in a Community Development area. <b>N/A</b>
Land Use & Planning	LU 3.1d	Create street and trail networks that directly connect local destinations, and that are friendly to pedestrians, equestrians, bicyclists, and others using non-motorized forms of transportation.	Regional, county and project trails are incorporated throughout the Kohl Ranch project site. All levels of street hierarchy include a sidewalk and/or trail to facilitate pedestrian travel and to permit pedestrian access to areas and trails within the community. <b>C</b>
Land Use &	LU 3.1e	Re-plan existing urban cores and specific plans for higher density, compact development as	It is the intent of this policy to achieve the RCIP vision for higher density and compact development by re-planning existing urban core and specific plan areas. Although the proposed project promotes



Section	General Plan Policy No.	General Plan Policies	Relationship of the Project to the Policy
Planning		appropriate to achieve the RCIP vision.	higher density, compact development as appropriate to achieve the RCIP vision, this project area for The Kohl Ranch is not part of an existing urban core. The Kohl Ranch Specific Plan Amendment No. 2 is proposing to reallocate land uses within an existing specific plan but not plan for a higher density. Accordingly, this policy is does not apply to The Kohl Ranch. <b>N/A</b>
Land Use & Planning	LU 3.1g	Provide the opportunity to link communities through access to multi-modal transportation systems.	See response to Policy LU 2.1.f, above. <b>C</b>
Land Use & Planning	LU 3.2	Use open space, greenways, recreational lands, and watercourses as community separators.	The Kohl Ranch will provide buffers and use of open space areas to create separation from land uses. <b>C</b>
Land Use & Planning	LU 4.1	Require that new developments be located and designed to visually enhance, not degrade the character of the surrounding area through consideration of the following concepts:	The Kohl Ranch Specific Plan has developed its own Design Guidelines. New developments will be required to comply with these guidelines to contribute to the visual order and consistency of the entire project site. <b>C</b>
Land Use & Planning	LU 4.1a	Compliance with the design standards of the appropriate area plan land use category.	The Kohl Ranch Specific Plan has developed its own Design Guidelines. The Thermal Design Guidelines were created and adopted a number of years after approval of the Kohl Ranch Specific Plan. The Thermal Design Guidelines would apply to the Kohl Ranch if the SP guidelines are silent. Thus, the project will adhere to the design standards approved for the area. <b>C</b>
Land Use & Planning	LU 4.1b	Require that structures be constructed in accordance with the requirements of the County's zoning, building, and other pertinent codes and regulations.	The Kohl Ranch Specific Plan has established its own set of zoning standards, and development will occur in accordance with those requirements and with all other applicable zoning regulations not in the specific plan. Building codes and other pertinent regulations will be applied to the project as it develops. <b>C</b>
Land Use & Planning	LU 4.1c	Require that an appropriate landscape plan be submitted and implemented for development projects subject to discretionary review.	Prior to the issuance of building permits, the Planning Dept. must approve plans for developed common open space areas, including landscaping and irrigation plans. Plans include final grading plans, irrigation plans certified by a landscape architect, certified landscape plans, fence treatment, and special treatment/buffer area treatment plans. The Kohl Ranch Specific Plan Development Standards support this. <b>C</b>
Land Use & Planning	LU 4.1d	Require that new development utilize drought tolerant landscaping and incorporate adequate drought-conscious irrigation systems.	The Kohl Ranch Specific Plan's Landscape Guidelines require the implementation of water use efficiency measures such as use of drought tolerant plants, grouping plants with similar irrigation requirements, use of mulch, and water efficient irrigation systems, consistent with County Ordinance 859, Coachella Valley Water District Landscape Ordinance 1302.1 and the Thermal Design Guidelines plant palette. <b>C</b>
Land Use & Planning	LU 4.1e	Pursue energy efficiency through street configuration, building orientation, and landscaping to capitalize on shading and facilitate solar energy, as provided for in Title 24 of the California Administrative Code.	Passive energy efficiency techniques will occur within the proposed The Kohl Ranch Specific Plan, including orientation of buildings, planting trees to take advantage of sun and adequate roof overhangs. Active energy efficiency measures will be addressed and required in Title 24 for such methods as proper wall and ceiling insulation. <b>C</b>
Land Use & Planning	LU 4.1f	Incorporate water conservation techniques, such as groundwater recharge basins, use of porous pavement, drought tolerant landscaping, and water recycling, as appropriate.	The Kohl Ranch Specific Plan contains water conservation measures that incorporate a dual water system consisting of installation of potable water lines for domestic purposes and to provide water for fire protection. Non-potable water lines will be installed for irrigation purposes and drought tolerant landscaping will be implemented throughout the Specific Plan. <b>C</b>
Land Use & Planning	LU 4.1g	Encourage innovative and creative design concepts.	The Kohl Ranch Specific Plan has developed its own Design Guidelines. New developments will be required to comply with these guidelines to contribute to the visual order and consistency of the entire project site. <b>C</b>

Section	General Plan Policy No.	General Plan Policies	Relationship of the Project to the Policy
Land Use & Planning	LU 4.1g	Encourage the provision of public art.	Although not specifically discussed in the Specific Plan, public art is not discouraged. As individual development proposals are reviewed by the County, public art could be included. <b>C</b>
Land Use & Planning	LU 4.1i	Include consistent and well-designed signage that is integrated with the building's architectural character.	At the specific plan level, no particular signs on buildings are proposed. General signage guidelines are included within the Specific Plan and are limited in scope but will conform to the project's design guidelines and Riverside County Ordinance 348. <b>C</b>
Land Use & Planning	LU 4.1j	Provide safe and convenient vehicular access and reciprocal access between adjacent commercial uses.	The project's circulation system will be designed to provide safe and convenient vehicular access to all commercial uses that complies with Riverside County Transportation Department and Fire Department requirements. During future site plan reviews assessments can be made for the possibility of reciprocal access between adjacent uses. <b>C</b>
Land Use & Planning	LU 4.1k	Locate site entries and storage bays to minimize conflicts with adjacent residential neighborhoods.	At the time of site plan review the Riverside County Planning Department will apply all applicable design standards to minimize conflicts with adjacent residential neighborhoods. <b>C</b>
Land Use & Planning	LU 4.1l	Mitigate noise, odor, lighting, and other impacts on surrounding properties.	To the extent feasible at the specific plan level, the EIR has found these types of impacts to be less than significant or reduced to less than significant through mitigation included in the EIR. <b>C</b>
Land Use & Planning	LU 4.1m	Provide and maintain landscaping in open spaces and parking lots.	The Design Guidelines of The Kohl Ranch Specific Plan provide guidance for designing open space landscaping in common areas. The Specific Plan requires that a special district or Home Owners Association be established to maintain all common landscape areas. Parking lots are required to be landscaped in accordance with Riverside County Ordinance No. 348 requirements. <b>C</b>
Land Use & Planning	LU 4.1n	Include extensive landscaping.	The Kohl Ranch Specific Plan has developed unique Design Guidelines which will encourage appropriate landscaping. The tree planting and landscaping program in The Kohl Ranch Specific Plan is proposed to be extensive with the use of drought tolerant and non-invasive plant species. The proposed The Kohl Ranch Specific Plan will adhere to standards detailed in the Specific Plan's landscaping standards and guidelines, and in Riverside County Ordinance Nos. 348 and 859 as well as the Thermal Design Guidelines and Coachella Valley Water District Landscape Ordinance 1302.1. <b>C</b>
Land Use & Planning	LU 4.1o	Preserve natural features, such as unique terrain, drainage ways, and native vegetation, wherever possible, particularly where they provide continuity with more extensive regional systems.	The Kohl Ranch Specific plan lies on relatively flat terrain with high disturbed soils due to past and present agriculture. <b>N/A</b>
Land Use & Planning	LU 4.1p	Require that new development be designed to provide adequate space for pedestrian connectivity and access, recreational trails, vehicular access and parking, supporting functions, open space and other pertinent elements.	Approximately 376 acres of open space will be provided within the Project for use as trails and parks. An extensive trail system connects parks, open space, and community facilities. Landscaped streets include sidewalks for pedestrian access. The residential community will have access to sidewalks and on-street bike lanes and off-street bike paths within the Project site. <b>C</b>
Land Use & Planning	LU 4.1q	Design parking lots and structures to be functionally and visually integrated and connected.	Development within the Kohl Ranch will comply with the specific plan's design guidelines and applicable Riverside County zoning ordinance requirements regarding the design and screening of parking areas. <b>C</b>
Land Use & Planning	LU 4.1r	Site building access points along sidewalks, pedestrian areas, and bicycle routes, and include amenities that encourage pedestrian activity.	The Design Guidelines encourage buildings to be oriented toward and visible from the street and parking areas to provide for easy access to bicycle routes and sidewalks. Courtyards and outdoor seating areas are encouraged as well as other ancillary structures and service areas. <b>C</b>
Land Use & Planning	LU 4.1s	Establish safe and frequent pedestrian crossings.	Internal sidewalks will connect The Kohl Ranch Specific Plan development to internal parks and to other project areas. Pedestrian connectivity and movement is emphasized in street and parking areas designs with numerous crossing points at parking and street intersections. <b>C</b>

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Land Use & Planning	LU 4.1t	Create a human-scale ground floor environment that includes public open space areas that separate pedestrian space from auto traffic and where mixed, it does so with special regard to pedestrian safety.	Courtyards and outdoor seating areas are encouraged as well as other ancillary structures and service areas. <b>C</b>
Land Use & Planning	LU 4.2	Require property owners to maintain structures and landscaping to a high standard of design, health, and safety through the following:	The Kohl Ranch Specific Plan Design Guidelines will complement the County-wide Design Guidelines applicable to this area. <b>C</b>
Land Use & Planning	LU 4.2a	Compliance with the design standards of the appropriate area plan land use category.	The Kohl Ranch Specific Plan Design Guidelines will complement the County-wide Design Guidelines applicable to this area. <b>C</b>
Land Use & Planning	LU 4.2b	Promote programs and work with local service organizations and educational institutions to inform residential, commercial, and industrial property owners and tenants about property maintenance methods.	The Kohl Ranch Specific Plan requires that a Homeowners' Association shall be established for the specific plan area, to assume ownership and maintenance responsibility for all common recreation, open space, private circulation systems within gated communities and within multi-family complexes, and landscaped areas. The Homeowners' Association will enforce any property and landscaping maintenance requirements established by the association and as appropriate will provide homeowners with information regarding maintenance methods. <b>C</b>
Land Use & Planning	LU 4.2c	Promote and support community and neighborhood based efforts for the maintenance, upkeep, and renovation of structures and sites.	Please see discussion of Policy LU 4.2.b, above. <b>C</b>
Land Use & Planning	LU 6.1	Require land uses to develop in accordance with the General Plan and area plans to ensure compatibility and minimize impacts.	See discussions related to General Plan consistency under Policy LU 2.1, above. Setbacks and buffers incorporated in the specific plan and mitigation measures included in the EIR reduce all potential significant impacts to less than significant levels which ensure compatibility issues are minimized. <b>C</b>
Land Use & Planning	LU 6.3	Consider the positive characteristics and unique features of the project site and surrounding community during the design and development process.	The Kohl Ranch Specific Plan has developed its own Design Guidelines. New developments will be required to comply with these guidelines to contribute to the visual order and consistency of the entire project site. <b>C</b>
Land Use & Planning	LU 6.4	Retain and enhance the integrity of existing residential, employment, agricultural, and open space areas by protecting them from encroachment of land uses that would result in impacts from noise, noxious fumes, glare, shadowing, and traffic.	Currently, the Kohl Ranch Specific Plan project site and surrounding lands are currently used for open farmland, agricultural activities and rural residential living. However, the Eastern Coachella Valley area is rapidly developing with residential land uses in the cities of Coachella, La Quinta, Indio Palm Desert. Design Guidelines of the Specific Plan will provide for the buffering of the surrounding community from this development. <b>C</b>
Land Use & Planning	LU 6.5	Require buffering to the extent possible between urban uses and adjacent rural/equestrian oriented land uses.	The Kohl Ranch Specific Plan contains a regional trail which may be used for equestrian purposes that will continue to provide linkages to some of the rural/equestrian oriented land uses; and the Specific Plan proposes various trails along some of the perimeters of the project site adjacent to existing rural uses to preserve access to open space areas and offer a buffer between the project's urban development and those uses. In addition, The Kohl Ranch will be buffered from the existing rural and equestrian properties through the use of equestrian trails, open space areas, and site-specific designs. A 300-foot buffer between project development and active agricultural uses is required by mitigation within this DEIR. <b>C</b>
Land Use & Planning	LU 6.6	Require buffering between urban uses and adjacent rural/equestrian oriented land uses.	Please see discussions of LU6.5 above. <b>C</b>
Land Use & Planning	LU 7.1	Accommodate the development of a balance of land uses that maintain and enhance the County's fiscal viability, economic diversity, and environmental integrity.	The Kohl Ranch is a primarily residential specific plan, with some commercial, industrial and public facilities uses also allowed. The Fiscal Impact Report for this specific plan shows that the project will have a positive impact the County's General Fund, and other public facilities funds, thus enhancing the County's fiscal viability. The project allows for the development of approximately 379,000 square feet of retail/commercial uses and 6,486,000 square feet of industrial uses. The environmental integrity

Section	General Plan Policy No.	General Plan Policies	Relationship of the Project to the Policy
			of the project area and the County is being preserved through the conservation of 376 acres of open space. <b>C</b>
Land Use & Planning	LU 7.2	Promote and market the development of a variety of stable employment and business uses that provide a diversity of employment opportunities.	The Kohl Ranch Specific Plan will include approximately 379,000 square feet of commercial/retail development and 6,486,000 square feet of industrial development, which would provide the area with new businesses and increase employment opportunities. <b>C</b>
Land Use & Planning	LU 7.3	Promote the development of focused employment centers rather than inefficient strip commercial development.	The Kohl Ranch Specific Plan proposal includes approximately 379,000 square feet of commercial development within an area comprised of 31.06 acres along Avenue 62. The Specific Plan designates four locations within the 31.06 acres for the commercial uses. The placement of the commercial development will be concentrated at the four proposed locations and the surrounding area will be developed with medium density, medium high density and very high density residential. The focused commercial development avoids the strip commercial concept and meets the intent of Policy LU 7.3. <b>C</b>
Land Use & Planning	LU 7.12	Improve the relationship and ratio between jobs and housing so that residents have an opportunity to live and work within the County.	The Kohl Ranch Specific Plan will include approximately 379,000 square feet of commercial development, which would provide the area with new businesses and increase employment opportunities for residents within the project area. <b>C</b>
Land Use & Planning	LU 8.1	Provide for permanent preservation of open space lands that contain important natural resources, hazards, water features, watercourses, and scenic and recreational values.	The Kohl Ranch Specific Plan includes no important natural resources. <b>N/A</b>
Land Use & Planning	LU 8.2	Require that development protect environmental resources by compliance with the Multipurpose Open Space Element of the General Plan and Federal and State regulations such as CEQA, NEPA, the Clean Air Act, and the Clean Water Act.	The project is in compliance with all applicable federal, state and local regulations that protect environmental resources, including the Western Riverside MSHCP, the General Plan's Open Space Element, Clean Air Act and the Clean Water Act. The proposed Specific Plan and all future implementing development proposals will comply with applicable provisions of the California Environmental Quality Act (CEQA). <b>C</b>
Land Use & Planning	LU 8.3	Incorporate open space, community greenbelt separators, and recreational amenities into Community Development areas in order to enhance recreational opportunities and community aesthetics, and improve the quality of life.	The Kohl Ranch Specific Plan proposes to establish 376 acres of Open Space with trails. Additionally, the Specific Plan includes parks which could include play equipment, pool, picnic tables, and passive play areas. These amenities will enhance recreational opportunities and community aesthetics and improve the quality of life compliant with Policy LU 8.3. A potential lake is proposed in Neighborhoods K and J which will be used as part of the dual water system and part of the projects drainage network but will provide a scenic amenity and enhance recreational opportunities by providing the use of a project trails surrounding the lakes. <b>C</b>
Land Use & Planning	LU 8.4	Allow development clustering and/or density transfers in order to preserve open space, natural resources, and/or biologically sensitive resources.	The project proposes to cluster development into medium-high, high density, and very-high residential housing in part to address airport master plan requirements. The increase in residential density and clustering will allow for preservation of 376 acres of open space, however no natural resources needing preservation exist on-site. <b>C</b>
Land Use & Planning	LU 8.5	Prior to the approval of any residential project, require that the project site be annexed into an existing parks and recreation district or CSA providing for neighborhood and community park maintenance.	At the time of recordation of final subdivision map(s), the applicant and/or developer shall convey common greenbelt, common open space areas and neighborhood parts to the master property owners association or appropriate public maintenance agency. <b>C</b>
Land Use & Planning	LU 9.1	Require that new development contribute their fair share to fund infrastructure and public facilities such as police and fire facilities.	The Kohl Ranch will be required to pay its fair share of public safety and infrastructure-related mitigation fees as per Riverside County Ordinance No. 659.6. Additionally, the proposed project will allow for the development of public facilities within the project area. <b>C</b>
Land Use & Planning	LU 9.2	Require a fiscal impact analysis for specific plans and major development proposals so as not to have a negative fiscal impact on the County.	The Fiscal Impact Report shows a net fiscal surplus for the County as a result of implementation of the proposed project. <b>C</b>

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Land Use & Planning	LU 10.4	Provide options to the automobile in communities, such as transit, bicycle and pedestrian trails, to help improve air quality.	The Kohl Ranch Specific Plan proposes sidewalks along all streets and an extensive network of trails along most major roadways as a way to offer pedestrian and bicycle "friendly" means of travel with the intent of providing options to the use of the automobile for internal project area travel as a way to help improve air quality. <b>C</b>
Land Use & Planning	LU 16.4	Encourage conservation of productive agricultural lands. Preserve prime agricultural lands for high-value crop production.	The Eastern Coachella Valley Area Plan contains 41,403 acres of AG designated land which is 9.1 percent of all the land comprising this Area Plan. Within Riverside County, only the San Jacinto Valley Area Plan designates a greater percentage of land to AG with 9.4 percent. The majority of the Eastern Coachella Valley AG designated land is concentrated along the northern boundary of Lakeview area and separated from other communities by dedicated conservation lands or park lands. The Kohl Ranch Specific Plan would not retain the Agriculture (AG) designated lands within its project area. Section V.C.2 of the EIR analyzed impacts and determined these to be significant and unavoidable as it eliminated 1,468 acres of Prime Farmland, 171 acres of Farmland of Statewide Importance and 480 acres of Farmland of Local Importance. A statement of overriding considerations was adopted with certification of the EIR November 16, 1999. A General Plan Amendment was approved to change land use to that of an adopted specific plan and a zone change changed the zoning to Specific Plan (SP). This policy is therefore, not applicable to the Kohl Ranch Specific Plan Amendment No. 2 with the exception of minimizing impacts to agriculturally designated lands from incompatible uses. EIR section V.C.2 incorporates mitigation requiring adherence to Ordinance 625 to protect agricultural uses. To avoid potential impact, a 300-foot setback between development and offensive agricultural uses is required as mitigation. <b>N/A, C</b>
Land Use & Planning	LU 16.5	Continue to participate in the California Land Conservation Act (the Williamson Act) of 1965.	There are currently no Williamson Act contracts on any properties within The Kohl Ranch Specific Plan. <b>N/A</b>
Land Use & Planning	LU 16.6	Require consideration of State agricultural land classification specifications when a 2½-year Agriculture Foundation amendment to the General Plan is reviewed that would result in a shift from an agricultural to a non-agricultural use.	The Kohl Ranch Specific Plan would not retain the Agriculture (AG) designated lands within its project area. Section V.C.2 of the EIR analyzed impacts and determined these to be significant and unavoidable. A statement of overriding considerations was adopted with certification of the EIR November 16, 1999. Subsequently, the zoning was changed to Specific Plan (SP). This policy is therefore, not applicable to the Kohl Ranch Specific Plan Amendment No. 2. <b>N/A</b>
Land Use & Planning	LU 16.7	Adhere to Riverside County's Right-to-Farm Ordinance.	Riverside County Ordinance No. 625 (Right-to-Farm Ordinance) pertains to residential subdivisions that are to be located within 300 feet of properties zoned for agricultural uses (A-1 (Light Agriculture), A-2 (Heavy Agriculture), A-P (Agriculture-Poultry), A-D (Agriculture-Dairy), or C/V (Citrus/Vineyard). The EIR analyzed impacts in section V.C.2 to such lands and incorporates mitigation measures to ensure adherence to Ordinance 625. <b>C</b>
Land Use & Planning	LU 17.1	Require that grading be designed to blend with undeveloped natural contours of the site and avoid an unvaried, unnatural, or manufactured appearance.	The project area is relatively flat and will not encroach upon hillsides. <b>N/A</b>
Land Use & Planning	LU 17.3	Ensure that development does not adversely impact the open space and rural character of the surrounding area.	The project area is relatively flat and because of the limited development on the site, views of the nearby mountains framing the valley floor are generally unobstructed. Buffers between land uses will allow for appropriate transitions to surrounding uses and compatibility issues can be addressed to retain rural character adjacent to existing rural areas. The intensity of the development will also place a greater number of people within easy reach of open space. Over 376 acres of open space will be preserved as part of this project. As part of the management of the open space, limited recreational uses will be allowed within this area (trails, horseback riding, etc.). <b>C</b>
Land Use & Planning	LU 17.4	Encourage clustered development where appropriate on lots smaller than the underlying land use designation would allow. While lot sizes may vary, the overall project density must not exceed that of the underlying land use designation unless associated with an incentive	The Kohl Ranch Specific Plan Amendment No. 2 is a reallocation of land uses allowing for higher density residential designations however, the overall density will not exceed the previously approved

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		program.	maximum allotted dwelling units approved under the original Specific Plan November 16, 1999. <b>C</b>
Land Use & Planning	LU 17.5	Encourage parcel consolidation.	The master developer for The Kohl Ranch has assembled approximately 2,163 acres to form the project site. This consolidation of parcels under one control offers the ability to preserve 376 acres of open space as envisioned by the CVMSHCP. <b>C</b>
Land Use & Planning	LU 18.1	Require that structures be designed to maintain the environmental character in which they are located.	Developments within the Project site will adhere to the Kohl Ranch Specific Plan Design Guidelines which address and respect the desert environment. <b>C</b>
Land Use & Planning	LU 22.1	Accommodate the development of single- and multi-family residential units in areas appropriately designated by the General Plan and area plan land use maps.	The Kohl Ranch provides a range of densities that will allow for both single- and multi-family residences to be built within the specific plan area which is consistent with the General Plan vision and the Community Development Foundation. <b>C</b>
Land Use & Planning	LU 22.2	Accommodate higher density residential development near community centers, transportation centers, employment, and services areas.	The very high density residential designation is located in close proximity to commercial and industrial land uses to locate near employment, service and transit centers. <b>C</b>
Land Use & Planning	LU 22.3	Require that adequate and available circulation facilities, water resources, and sewer lines exist to meet the demands of the proposed residential land use. (AI 3)	At the present time there are not adequate facilities available to meet the needs of The Kohl Ranch. However, through County regulations and mitigation measures, which include the payment of development impact fees, and the construction of utility facilities, facilities can be constructed to meet the needs of the proposed project. The Water Source Assessment prepared for this project indicates that water resources are available to serve this project. Some water and wastewater facilities have already been constructed and all necessary infrastructure is planned. <b>C</b>
Land Use & Planning	LU 22.4	Accommodate the development of a variety of housing types, styles and densities that are accessible to and meet the needs of a range of lifestyles, physical abilities, and income levels.	The Kohl Ranch Specific Plan consists of medium (2 to 5 du/ac), medium-high (5 to 8 du/acre), high density (8 to 14 du/acre), and very high density (14 to 20 du/ac) residential uses. It is intended that the Specific Plan, through the various residential densities, will cater to a range of lifestyles, physical abilities, and income levels. <b>C</b>
Land Use & Planning	LU 22.5	Integrate a continuous network of parks, plazas, public squares, bicycle trails, transit systems, and pedestrian paths to provide both connections within each community and linkages with surrounding features and communities.	The project includes a trail system. This system will include a combination of greenbelts with linkages and sidewalks located adjacent to roads and within roadways. It will incorporate trails for pedestrians, bicyclists, and equestrians in the form of an integrated system of hard- and soft-surface trails. The trail system will promote non-vehicular access to on-site recreational areas as well as to the schools. The project design concept is that of a pedestrian-friendly lifestyle which includes trail systems located throughout the project area that connect schools, services, goods, parks, and trailheads. <b>C</b>
Land Use & Planning	LU 22.6	Require setbacks and other design elements that buffer residential units from the impacts of abutting agricultural, roadway, commercial, and industrial uses. (AI 3)	The project design concepts include landscaping and setbacks to buffer residential uses from roads. Drainage facilities are also used as landscaped paseo areas to separate residential areas from roads and other uses. <b>C</b>
Land Use & Planning	LU 22.7	Allow for reduced street widths to minimize the influence of the automobile and improve the character of a neighborhood, in accordance with the Riverside County Fire Department.	Internal roadways within residential tracts of the specific plan may have narrow private streets, the widths of which will be determined in coordination with County Transportation Department and the Riverside County Fire Department. <b>C</b>
Land Use & Planning	LU 22.8	Establish activity centers within or near residential neighborhoods that contain services such as child or adult-care, recreation, public meeting rooms, convenience commercial uses, or similar facilities.	The Kohl Ranch Specific Plan includes commercial and residential land uses. It is foreseeable that the development of this area could include services as desired in this policy. <b>C</b>
Land Use & Planning	LU 22.9	Require residential projects to be designed to maximize integration with and connectivity to nearby community centers, rural villages, and neighborhood centers.	The project includes commercial uses that would be well-integrated into the residential area. Additionally, there are internal trails and roadway that will connect the project site to the surrounding community. <b>C</b>

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Land Use & Planning	LU 22.10	Require that residential units/projects be designed to consider their surroundings and to visually enhance, not degrade, the character of the immediate area.	All buildings in the proposed The Kohl Ranch Specific Plan will utilize elements of architectural styles described in the Specific Plan's Design Guidelines and will enhance the character of the area. With sufficient buffering, landscaping, and separation from the adjacent properties as described under previous policies, the project would not degrade the character of the immediate area. <b>C</b>
Land Use & Planning	LU 25.1	Accommodate the development of public facilities in areas appropriately designated by the General Plan and area plan land use maps.	Planning Area M-4 is designated for Public Facilities as this site houses three schools. <b>C</b>
Land Use & Planning	LU 25.2	Protect major public facilities, such as landfill and solid waste disposal sites and airports, from the encroachment of incompatible uses.	The project is located adjacent to the Jacqueline Cochran Regional Airport. The Airport Land Use Commission has found the Kohl Ranch Specific Plan to be conditionally consistent with the 2005 Jacqueline Cochran Regional Airport (formerly Desert Resorts Regional Airport) Land Use Compatibility Plan (JCRALUCP). EIR Section V.D.12 analyzed and incorporates mitigation measures to ensure compliance with the JCRALUCP. <b>C</b>
Land Use & Planning	LU 25.3	Require that new public facilities protect sensitive uses, such as schools and residences, from the impacts of noise, light, fumes, odors, vehicular traffic, parking, and operational hazards.	Public facilities that would be constructed within The Kohl Ranch Specific Plan are operated by Coachella Valley Water District and Riverside County Flood Control District. Implementing projects may require public facilities in the future, such as drainage facilities. Those facilities will be designed to have minimal impact to the surrounding land uses, and thus will comply with this policy. Construction and operations of currently anticipated major facilities have been evaluated in EIR. <b>C</b>
Land Use & Planning	LU 25.4	Require that adequate and available circulation facilities, water resources, and sewer facilities exist to meet the demands of the proposed land use.	At the present time there are not adequate facilities available to meet the needs of The Kohl Ranch. However, through County regulations and mitigation measures, which include the payment of development impact fees, and the construction of utility facilities, facilities can be constructed to meet the needs of the proposed project. The Water Source Assessment prepared for this project indicates that water resources are available to serve this project. Some water and wastewater facilities have already been constructed and all necessary infrastructure is planned. <b>C</b>
Noise	N 1.1	Protect noise-sensitive land uses from high levels of noise by restricting noise producing land uses from these areas. If the noise producing land use cannot be relocated, then noise buffers such as setbacks, landscaping, or block walls shall be used.	The Eastern Coachella Valley area is a rapidly developing area. The Design Guidelines of the Specific Plan will provide for the buffering of the surrounding community from this development. Industrial and commercial uses are planned nearest the airport and clustering of residential units will allow all airport noise related issues to be addressed/avoided. <b>C</b>
Noise	N 1.2	Guide noise tolerant land uses into areas irrevocably committed to land uses that are noise producing, such as transportation corridors, or within the projected noise contours of any adjacent airports.	Industrial and commercial uses are planned nearest the airport and clustering of residential units will allow all airport noise related issues to be addressed/avoided. Community walls, landscaping, setbacks and drainage facilities are used to buffer residential uses from surrounding roads. The EIR and Addendum incorporate mitigation measures to reduce these impacts to less than significant. <b>C</b>
Noise	N 1.5	Prevent and mitigate the adverse impacts of excessive noise exposure on the residents, employees, visitors, and noise sensitive uses of Riverside County.	The project will mitigate adverse impacts of excessive noise to residents, employees, visitors, and noise sensitive uses of Riverside County. Once precise grading and architectural plans are made available, a final acoustical study will be performed to confirm the appropriate noise standard levels for sensitive receptors. To retain visibility and access, setbacks, berms, and walls may be used to achieve acceptable noise levels. <b>C</b>
Noise	N 4.2	Develop measures to control non-transportation noise impacts.	The exact type of retail/commercial uses that will be occupying the commercial land is unknown at this time; therefore, the types of noise generated by the on-site businesses cannot be determined. However, once precise grading and architectural plans are made available, a final acoustical study will be performed to confirm the appropriate noise standard levels for sensitive receptors. To retain visibility and access, setbacks, berms, and walls may be used to achieve acceptable noise levels. (The concurrently proposed Thermal Motorsports Park has provided such documentation and been made consistent with this policy.) <b>C</b>

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Noise	N 4.3	Ensure any use determined to be a potential generator of significant stationary noise impacts be properly analyzed, and ensure that the recommended mitigation measures are implemented.	The exact type of retail/commercial uses that will be occupying the commercial land is unknown at this time; therefore, the types of noise generated by the on-site businesses cannot be determined. However, once precise grading and architectural plans are made available, a final acoustical study will be performed to confirm the appropriate noise standard levels for sensitive receptors. To retain visibility and access, setbacks, berms, and walls may be used to achieve acceptable noise levels. (The concurrently proposed Thermal Motorsports Park has provided such documentation and been made consistent with this policy.) <b>C</b>
Noise	N 8.3	Require development that generates increased traffic and subsequent increases in the ambient noise level adjacent to noise-sensitive land uses to provide for appropriate mitigation measures.	The project will mitigate adverse impacts of excessive noise to residents, employees, visitors, and noise sensitive uses of Riverside County. Once precise grading and architectural plans are made available, a final acoustical study will be performed to confirm the appropriate noise standard levels for sensitive receptors. To retain visibility and access, setbacks, berms, and walls may be used to achieve acceptable noise levels. (The concurrently proposed Thermal Motorsports Park has provided such documentation and been made consistent with this policy.) <b>C</b>
Noise	N 11.1	Utilize natural barriers such as hills, berms, boulders, and dense vegetation to assist in noise reduction.	The Design Guidelines of the Specific Plan will provide for the buffering methods which do utilize such "natural barriers." <b>C</b>
Noise	N 12.1	Minimize the impacts of construction noise on adjacent uses within acceptable practices.	The project will be subject to Riverside County Ordinance No. 457, which regulates construction practices; and Riverside County Ordinance No. 847, which regulates noise. <b>C</b>
Noise	N 12.2	Ensure that construction activities are regulated to establish hours of operation in order to prevent and/or mitigate the generation of excessive or adverse noise impacts on surrounding areas.	The project will be subject to Riverside County Ordinance No. 457, which regulates construction practices; and Riverside County Ordinance No. 847, which regulates noise. <b>C</b>
Noise	N 12.4	Require that all construction equipment utilize noise reduction features (e.g. mufflers and engine shrouds) that are no less effective than those originally installed by the manufacturer.	The mitigation measures of the noise section of the EIR and Addendum require that all construction equipment shall be required to minimize noise from construction activities. All construction will be required to comply with local noise control ordinances. <b>C</b>
Noise	N 14.1	Minimize the potential adverse noise impacts associated with the development of mixed-use structures where residential units are located above or adjacent to commercial uses.	Mixed use structures are not proposed in the Kohl Ranch Specific Plan. <b>N/A</b>
Noise	N 14.2	Require that commercial and residential mixed use structures minimize the transfer or transmission of noise and vibration from the commercial land use to the residential land use.	Mixed use structures are not proposed in the Kohl Ranch Specific Plan. <b>N/A</b>
Noise	N 14.3	Minimize the generation of excessive noise level impacts from entertainment and restaurant/bar establishments into adjacent residential or noise sensitive uses.	The exact type of retail/commercial uses that will be occupying the commercial land uses is unknown at this time; therefore, the types of noise generated by the on-site businesses cannot be determined. However, once precise grading and architectural plans are made available, a final acoustical study will be performed to confirm the appropriate noise standard levels for sensitive receptors. To retain visibility and access, setbacks, berms, and walls may be used to achieve acceptable noise levels. <b>C</b>
Noise	C 3.27	Evaluate proposed highway extensions or widening projects for potential noise impacts on existing and future land uses in the area. Require that the effects of truck mix, speed limits, and ultimate motor vehicle volumes on noise levels are also explored during the environmental process.	The exterior noise impacts from traffic to future land uses have been calculated. The Addendum has incorporated mitigation measures regarding the necessary height of noise barriers to obtain the appropriate noise standard levels for sensitive receptors and commercial land uses. <b>C</b>
Population/Housing	5 Year Action Plan: Policy 1.2	Ensure the availability of Suitable sites for the development of affordable housing to meet the needs of all household income levels, including farm workers and other special needs populations.	The Kohl Ranch Specific Plan consists of medium (2 to 5 du/ac), medium-high (5 to 8 du/acre), high density (8 to 14 du/acre), and very high density (14 to 20 du/ac) residential uses. It is intended that the Specific Plan, through the various residential densities, will cater to a range of lifestyles, physical abilities, and income levels. <b>C</b>



Section	General Plan Policy No.	General Plan Policies	Relationship of the Project to the Policy
Population/Housing	5 Year Action Plan: Policy 1.7	Encourage innovative housing, site plan design and construction techniques to promote new affordable housing by the private sector.	The Kohl Ranch Specific Plan consists of medium (2 to 5 du/ac), medium-high (5 to 8 du/acre), high density (8 to 14 du/acre), and very high density (14 to 22 du/ac) residential uses. It is intended that the Specific Plan, through the various residential densities, will cater to a range of lifestyles, physical abilities, and income levels. <b>C</b>
Population/Housing	5 Year Action Plan: Policy 1.7	Action 1.7a Continue to provide for greater flexibility in the design of single family development through the processing of PDs, Specific Plans, and Area Plans, and application of density bonus provisions, when requested, to allow for varying lot sizes and development standards than normally required in residential districts.	The Kohl Ranch is a Specific Plan which allows for a greater degree of flexibility in lot sizes, density bonuses, and housing product types than is the historical norm in Riverside County. <b>C</b>
Population/Housing	5 Year Action Plan: Policy 1.7	Action 1.7d Encourage new large scale development proposals to provide a range of housing types and densities for all income levels through the use of creative planning concepts as specific plans and mixed-use development.	The Kohl Ranch is a large-scale specific plan which introduces a wide variety of housing types, lot sizes, and densities. The Administrative Section of the specific plan allows for flexibility in product type, lot sizes, and densities. <b>C</b>
Population/Housing	5 Year Action Plan: Policy 5.1	Encourage the use of energy conservation features in residential construction and remodeling.	Passive energy efficiency techniques will occur within the proposed project, including orientation of buildings, planting trees to take advantage of sun and adequate roof overhangs. Active energy efficiency measures will be addressed and required in Title 24 for such methods as proper wall and ceiling insulation. <b>C</b>
Public Services	LU 5.1	Ensure that development does not exceed the ability to adequately provide supporting infrastructure and sheriff services.	Through the payment of Riverside County's required development impact fees (Ordinance No. 659), the implementation of the Periodic Medical Needs Assessment, which is required by Mitigation Measure 4.15.7A of the County General Plan EIR, and the implementation of mitigation measures listed in the EIR, the impacts of the Kohl Ranch Specific Plan to public facilities and services will be less than significant. <b>C</b>
Recreation	OS 20.4	Provide for the needs of all people in the system of County recreation sites and facilities, regardless of their socioeconomic status, ethnicity, physical capabilities or age.	The proposed project includes the construction of many types of trail systems, open space areas with passive and active uses available to the general public. The recreation plan is provided to the community as a whole and does not discriminate by socioeconomic status, ethnicity, physical capabilities or age. <b>C</b>
Recreation	OS 20.5	Require that development of recreation facilities occurs concurrent with other development in the area.	Recreational facilities will be built to satisfy the population demand of the proposed project, as referenced in the Kohl Ranch Specific Plan Development Standards. <b>C</b>
Recreation	OS 20.6	Require new development to provide implementation strategies for the funding of both active and passive parks and recreational sites.	The project will provide active parks and passive parks by design. If it is determined that park acreages are insufficient, fees will be paid in accordance with Riverside County Ordinance 460. <b>C</b>
Recreation	C 16.1	Implement the County trail system as depicted in the Bikeways and Trails Plan, Figure C-7.	The County trail system has been implemented in accordance with the Bikeways and Trails Plan and per the County Transportation Department and County Regional Parks and Recreation District. <b>C</b>
Recreation	C 16.2	Develop a multi-purpose recreational trail network with support facilities that provide a linkage with regional facilities.	The project design concept is that of a pedestrian-friendly lifestyle which includes trail systems located throughout the project area that connect schools, services, goods, parks, and trailheads. <b>C</b>
Recreation	C 16.3	Require that trail alignments either provide access to or link scenic corridors, schools, parks, and other natural areas.	The project design concept is that of a pedestrian-friendly lifestyle which includes trail systems located throughout the project area that connect schools, services, goods, parks, and trailheads. <b>C</b>
Recreation	C 17.3	Ensure that the bikeway system incorporates the following:a. Interconnection of cities and unincorporated communities;b. Provision of lanes to specific destinations such as state or county parks;c. Provision for bicycle touring; and,d. Encouragement of bicycle commuting.	The project design concept is that of a pedestrian-friendly lifestyle which includes trail systems located throughout the project area that connect schools, services, goods, parks, and trailheads. <b>C</b>
Transportation	C 2.1	Maintain the following countywide target Levels of Service: LOS "C" along all County	The project has been analyzed in the Traffic Study to meet the required LOS standards for the various

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		maintained roads and conventional state highways. As an exception, LOS "D" may be allowed in Community Development areas, only at intersections of any combination of Secondary Highways, Major Highways, Arterials, Urban Arterials, Expressways, conventional state highways or freeway ramp intersections. LOS "E" may be allowed in designated community centers to the extent that it would support transit-oriented development and walkable communities.	roadway types located within the project's sphere of influence. Section V.D.1 of the EIR incorporates mitigation measures to reduce impacts and ensure LOS standards meet required levels. <b>C</b>
Transportation	C 1.2	Support development of a variety of transportation options for major employment and activity centers including direct access to transit routes, primary arterial highways, bikeways, park-n-ride facilities, and pedestrian facilities.	The project includes a trail system. This system will include a combination of greenbelts with linkages and sidewalks located adjacent to roads and within roadways. It will incorporate trails for pedestrians, bicyclists, and equestrians in the form of an integrated system of hard- and soft-surface trails. The trail system will promote non-vehicular access to on-site recreational areas as well as to the schools. <b>C</b>
Transportation	C 1.5	Evaluate the planned circulation system as needed to enhance the arterial highway network to respond to anticipated growth and mobility needs.	The Traffic Study analyzed the planned circulation system with respect to the current County of Riverside General Plan Circulation Element and in coordination with future County efforts. <b>C</b>
Transportation	C 1.7	Encourage and support the development of projects that facilitate and enhance the use of alternative modes of transportation, including pedestrian-oriented retail and activity centers, dedicated bicycle lanes and paths, and mixed-use community centers.	The project design concept is that of a pedestrian-friendly lifestyle which includes trail systems located throughout the project area that connect schools, services, goods and parks and the various land uses. <b>C</b>
Transportation	C 2.2	Apply level of services standards to new development via a program establishing traffic study guidelines to evaluate traffic impacts and identify appropriate mitigation measures for new development.	The traffic analysis uses the Level of Service (LOS) system of categorization to evaluate the project area roadway intersections. Traffic engineers use this LOS system of categorization to describe how well an intersection or roadway is functioning. The LOS measures several factors including operating speeds, freedom to maneuver, traffic interruptions, and average vehicle delay at intersections. The LOS approach uses a ranking system, similar to education, with level 'A' being best and level 'F' being worst. <b>C</b>
Transportation	C 2.3	Traffic studies prepared for development entitlements (tracts, plot plans, public use permits, conditional use permits, etc.) shall identify project related traffic impacts and determine the "significance" of such impacts in compliance with CEQA.	Potential impacts related to inadequate parking capacity, altered waterborne, rail or air traffic were found to be less than significant in the Notice of Preparation for the Kohl Ranch Specific Plan. The subsequent Addendum for the Kohl Ranch Specific Plan Amendment No. 2 found no new or substantially increased significant effects. Some design features present on the current and future project roadways, such as curves, could result in potentially significant impacts however, all design features will be designed to meet County Standards. <b>C</b>
Transportation	C 2.4	The direct project related traffic impacts of new development proposals shall be mitigated via conditions of approval requiring the construction of any improvements identified as necessary to meet level of service standards.	The analysis shows that the project will contribute to the exceedance of acceptable levels of service for both intersections and roadway segments, and contribute to substantial increases in the traffic on roads. Mitigation measures in the form of signals and roadway improvements, and fair share fees, listed above, will be required to reduce these potentially significant impacts to below the level of significance. <b>C</b>
Transportation	C 2.5	The cumulative and indirect traffic impacts of development may be mitigated through the payment of various impact mitigation fees such as County Development Impact Fees, Road and Bridge Benefit District Fees, and Transportation Uniform Mitigation Fees to the extent that these programs provide funding for the improvement of facilities impacted by development.	To ensure that area-wide traffic conditions do not worsen as development occurs, the County of Riverside has established "fair share" mitigation fees, which include, but are not limited to, the Coachella Valley Association of Governments (CVAG) Transportation Uniform Mitigation Fee (TUMF), Development Impact Fees (DIF) and a Road and Bridge Benefit District (RBBD) fee, which is currently being established by the County, all described below. The project will participate in the cost of off-site improvements through payment of the following "fair share" mitigation fees: <ul style="list-style-type: none"> <li>• Coachella Valley Association of Governments Transportation Uniform Mitigation Fee (TUMF), current at time of construction</li> <li>• Riverside County Traffic Signal Systems Fee Program</li> <li>• RBBD (when approved and enacted)</li> </ul>

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			These fees shall be collected and utilized as needed by Riverside County to construct the improvements necessary to maintain the required level of service. <b>C</b>
Transportation	C 3.1	Design, construct, and maintain County roadways as specific in the County Road Improvement Standards and Specifications.	The Kohl Ranch Specific Plan includes Circulation Development Standards which are required of the project. <b>C</b>
Transportation	C 3.2	Maintain the existing transportation network, while providing for future expansion and improvement based on travel demand, and the development of alternative travel modes.	<p>To ensure that area-wide traffic conditions do not worsen as development occurs, the County of Riverside has established "fair share" mitigation fees, which include, but are not limited to, the Coachella Valley Association of Governments (CVAG) Transportation Uniform Mitigation Fee (TUMF), Development Impact Fees (DIF) and a Road and Bridge Benefit District (RBBD) fee, which is currently being established by the County, all described below. The project will participate in the cost of off-site improvements through payment of the following "fair share" mitigation fees:</p> <ul style="list-style-type: none"> <li>• Coachella Valley Association of Governments Transportation Uniform Mitigation Fee (TUMF), current at time of construction</li> <li>• Riverside County Traffic Signal Systems Fee Program</li> <li>• RBBD (when approved and enacted)</li> </ul> <p>These fees shall be collected and utilized as needed by Riverside County to construct the improvements necessary to maintain the required level of service. <b>C</b></p>
Transportation	C 3.10	Require private and public land developments to provide all on-site auxiliary facility improvements necessary to mitigate any development-generated circulation impacts. A review of each proposed land development project shall be undertaken to identify project impacts to the circulation system and its auxiliary facilities. The Transportation Department may require developers and/or subdividers to provide traffic impact studies prepared by qualified professionals to identify the impacts of a development.	The project has been analyzed in the Traffic Study to meet the required LOS standards for the various roadway types located within the project's sphere of influence. Where the LOS standards are expected to be exceeded, mitigation has been included that would reduce it to below the required levels. <b>C</b>
Transportation	C 3.13	Design street intersections, where appropriate, to assure the safe, efficient passage of through traffic and the negotiation of turning movements.	The Kohl Ranch project includes a General Plan Amendment which will modify the existing Circulation Element. The modified circulation plan provides improvement standards to promote efficient and safe movement of people within the project area. <b>C</b>
Transportation	C 3.14	Design curves and grades to permit safe movement of vehicular traffic at the road's design speed. Design speed should be consistent with and complement the character of the adjacent area.	The curvilinear roads proposed within the project site (Figure IV-2, The Kohl Ranch Specific Plan Circulation Plan) have been designed to meet the County Transportation Department's standards. <b>C</b>
Transportation	C 3.15	Provide adequate sight distances for safe vehicular movement at a road's design speed and at all intersections.	Sight distance at the project entrance roadways shall be reviewed with respect to standard County of Riverside sight distance standards at the time of preparation of final grading, landscape, and street improvement plans. <b>C</b>
Transportation	C 3.20	Determine location of General Plan road rights of way and levels of road improvements needed based primarily upon land uses and travel demand.	General Plan roadways and levels of road improvements are based on the Riverside County General Plan Circulation Element and coordination with the Riverside County Transportation Department and South Valley Implementation Plan. <b>C</b>
Transportation	C 3.24	Provide a street network with quick and efficient routes for emergency vehicles, meeting necessary street widths, turn-around radius, and other factors as determined by the Transportation Department in consultation with the Fire Department and other emergency service providers.	The Kohl Ranch Specific Plan includes Circulation Development Standards which are required of the project and are designed to meet County Transportation and Fire Department requirements. <b>C</b>
Transportation	C 3.26	Plan off-street parking facilities to support and enhance the concept of walkable and transit-oriented communities.	Section V.D.1 of the EIR incorporates mitigation to encourage ridesharing and transit ridership by requiring a portion of commercial parking areas be designated to Park and Ride. Additional mitigation

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			requires the coordination with areas bus transit service provider to incorporate bus stops and turnouts. <b>C</b>
Transportation	C 4.1	Provide facilities for the safe movement of pedestrians within developments, as specified in the County Ordinances Regulating the Division of Land of the County of Riverside.	The project includes a trail system. This system will include a combination of greenbelts with linkages and sidewalks located adjacent to roads. It will incorporate trails for pedestrians, bicyclists, and equestrians in the form of an integrated system of hard- and soft-surface trails. The trail system will promote non-vehicular access to on-site recreational areas as well as to the schools. <b>C</b>
Transportation	C 4.2	Maximize visibility and access for pedestrians and encourage the removal of barriers (walls, easements, and fences) for safe and convenient movement of pedestrians. Special emphasis should be placed on the needs of disabled persons considering Americans with Disabilities Act (ADA) regulations.	Sidewalks and trails will be incorporated in compliance with County standards. Special considerations for disabled persons were not addressed specifically at this level of analysis. <b>C</b>
Transportation	C 4.3	Assure pedestrian access from developments to existing and future transit routes and terminal facilities through project design.	The project includes a trail system. This system will include a combination of greenbelts with linkages and sidewalks located adjacent to roads. It will incorporate trails for pedestrians, bicyclists, and equestrians in the form of an integrated system of hard- and soft-surface trails. The trail system will promote non-vehicular access to on-site recreational areas as well as to the schools and future bus stops. <b>C</b>
Transportation	C 4.4	Plan for pedestrian access that is consistent with road design standards while designing street and road projects. Provisions for pedestrian paths or sidewalks and timing of traffic signals to allow safe pedestrian street crossing shall be included.	The project includes a trail system. This system will include a combination of greenbelts with linkages and sidewalks located adjacent to roads. It will incorporate trails for pedestrians, bicyclists, and equestrians in the form of an integrated system of hard- and soft-surface trails. The trail system will promote non-vehicular access to on-site recreational areas as well as to the schools and future bus stops. <b>C</b>
Transportation	C 4.9	Encourage, where feasible, the construction of overpasses or undercrossings where trails intersect arterials, urban arterials, expressways, or freeways.	Trails within the specific plan cross Avenue 62 which is classified as an expressway. It is infeasible to construct an undercrossing due to the high groundwater table in the area. Constructing an overcrossing which would need to span the 220-foot right-of-way and meet American's with Disabilities Act requirements would not be economically feasible. <b>NA</b>
Transportation	C 6.3	Limit access points and intersections of streets and highways based upon the road's General Plan classification and function. Access points must be located a sufficient distance away from major intersections to allow for safe, efficient operation.	Sight distance at the project entrance roadways shall be reviewed with respect to standard County of Riverside sight distance standards at the time of preparation of final grading, landscape, and street improvement plans. <b>C</b>
Transportation	C 7.2	Work with property owners to reserve right-of-way for potential CETAP corridors through site design, dedication, and land acquisition, as appropriate.	Three corridors are being examined in western Riverside County for the preservation of rights-of-way for future multi-modal transportation facilities. These include the Beaumont/Banning to Temecula transportation corridor, the Moreno Valley to San Bernardino corridor, and the Hemet to Corona/Lake Elsinore corridor. None of these corridors pass through the project area. <b>N/A</b>
Transportation	C 7.9	Review development applications in cooperation with RCTC and as appropriate, to identify the precise location of CETAP corridors and act to preserve such areas from any permanent encroachments, pending dedication or acquisition.	Three corridors are being examined in western Riverside County for the preservation of rights-of-way for future multi-modal transportation facilities. These include the Beaumont/Banning to Temecula transportation corridor, the Moreno Valley to San Bernardino corridor, and the Hemet to Corona/Lake Elsinore corridor. None of these corridors pass through the project area. <b>N/A</b>
Transportation	C 11.4	Offer incentives to new development to encourage it to locate in a transit-oriented area such as a community center or along a designated transit corridor near a station.	EIR section V.C.6 incorporates mitigation to encourage mixed-use services to reduce off-site vehicle trips. Commercial and Industrial uses are concentrated along Avenue 62 which is a main transit corridor and coordination efforts with SunLine Bus Transit are required to incorporate bus routes along this segment. <b>C</b>
Transportation	C 11.5	Accommodate transit through higher densities, innovative design, and right-of-way dedication.	Higher density residential land uses are proposed by the Kohl Ranch Specific Plan. These higher densities will facilitate the need for transit-oriented development. EIR section V.D.1 incorporates

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			mitigation to coordinate with SunLine Bus Agency to incorporate bus stops and turnouts. <b>C</b>
Transportation	C 11.7	Promote development of transit centers and park-n-rides for use by all transit operators, including development of multi-modal facilities.	Section V.D.1 of the EIR incorporates mitigation to encourage ridesharing and transit ridership by requiring a portion of commercial parking areas be designated to Park and Ride on weekdays between 6:00am and 6:00pm. Additional mitigation requires the coordination with areas bus transit service provider to incorporate bus stops and turnouts. <b>C</b>
Transportation	ECVAP 11.1	Design and develop the vehicular roadway system per Figure 8, Circulation, and in accordance with the functional classifications and standards in the System Design, Construction and Maintenance section of the General Plan Circulation Element.	Roadways have been designed in accordance with the standards in the System Design, Construction and Maintenance section of the General Plan Circulation Element. <b>C</b>
Transportation	ECVAP 11.2	Maintain the County's roadway Level of Service standards as described in the Level of Service section of the General Plan Circulation Element.	The project has been analyzed in the Traffic Study to meet the required LOS standards for the various roadway types located within the project's sphere of influence. Section V.D.1 of the EIR incorporates mitigation measures to reduce impacts and ensure LOS standards are below the required levels. <b>C</b>
Transportation	ECVAP 11.3	Separate vehicular traffic from pedestrian and equestrian traffic in order to avoid potential hazards and where traffic volumes justify the costs.	Trails are designed in accordance with Riverside County Transportation Department and County Regional Parks and Recreation District standards. <b>C</b>
Transportation	ECVAP 12.1	Coordinate with railroad companies to maintain and enhance railroad facilities south of the City of Coachella in accordance with the Rail System section of the General Plan Circulation Element.	The project does not propose the construction of new or modified existing rail line. The nearest rail line is over 7,500 feet from the project site. <b>N/A</b>
Transportation	ECVAP 12.2	Coordinate with railroad companies to encourage grade-separated crossings in and near Mecca.	The Kohl Ranch Specific Plan is not located in or near Mecca. <b>N/A</b>
Transportation	ECVAP 13.1	Implement the Trails and Bikeway System, Figure 9, as discussed in the Non-motorized Transportation section of the General Plan Circulation Element.	The County Trail and Bikeway System has been implemented in accordance with the Non-motorized Transportation section of the General Plan Circulation Element and per the County Transportation Department and County Regional Parks and Recreation District. <b>C</b>
Transportation	ECVAP 13.2	At signalized intersections, special equestrian push buttons (located at heights usable by persons riding on horseback) will be considered and installed where appropriate. Priority shall be given to those signalized intersections identified as trail crossings.	The inclusion of specialized push buttons would be determined at the time signalized intersections are proposed/built. There are no requirements or prohibitions in the specific plan for special equestrian push buttons. <b>C</b>
Transportation	ECVAP 13.3	As resources permit, consideration should be given to the placement of signs along those public rights-of-way identified as regional or community trail alignments alerting motorists to the possible presence of equestrian, bicycle and pedestrian (i.e., non-motorized) traffic.	The Kohl Ranch Specific Plan Design Guidelines incorporate requirements for placement of directional, traffic safety and public service signs. <b>C</b>
Transportation	LU 12.1	Provide land use arrangements that reduce reliance on the automobile and improve opportunities for pedestrian, bicycle, and transit use in order to minimize congestion and air pollution.	The Specific Plan proposes sidewalks along all streets and an extensive network of trails along most major roadways as a way to offer pedestrian and "bicycle-friendly" means of travel with the intent of providing alternatives the use of the automobile for internal project area travel as a way to help improve air quality. Trails will also encourage the use of bus transit. <b>C</b>
Transportation	LU 12.2	Locate employment and service uses in areas that are easily accessible to existing or planned transportation facilities.	Specific Plan is a concentrated residential development that proposes commercial and service uses near Avenue 62, an expressway. The SunLine Transit Agency does not currently provide bus service to this area however, section V.D.1 of the EIR incorporates mitigation to require coordination with the agency to incorporate bus stops and turnouts. Addendum Figure 15, provides potential stops and turnout locations based upon the most current circulation plan. Additionally, mitigation requires a portion of commercial parking areas be designated for Park and Ride facilities. <b>C</b>
Transportation	LU 12.3	Locate transit stations in community centers and at places of public, employment,	Addendum Figure 15 identifies potential bus stops and turnouts in areas designated for commercial and industrial uses where employment, public and entertainment concentrations are expected to

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		entertainment, recreation, and residential concentrations.	occur as well as areas designated for residential use. Additionally, section V.D.1 of the EIR incorporates mitigation to require coordination with the SunLine Transit Agency to incorporate bus stops and turnouts. Further, mitigation requires a portion of commercial parking areas be designated for Park and Ride facilities. <b>C</b>
Transportation	LU 12.4	Incorporate safe and direct multi-modal linkages in the design and development of projects, as appropriate.	The proposed project has been designed to incorporate sidewalks for pedestrians along all roadways. This system will provide the members of community an alternative method of travel in order to reduce residents' reliance on the automobile to reach these destinations. Vehicular access to all portions of the project site will be designed and developed in compliance with Riverside County standards. An extensive system of trails for bikes and pedestrians connects residential areas to local services and shopping, as well as to regional trails and open space. <b>C</b>
Transportation	LU 12.5	Allow traffic-calming elements, such as narrow streets, curb bulbs, textured paving, and landscaping, where appropriate.	Traffic-calming elements such as curvilinear streets and cul-de-sacs are encouraged and proposed within The Kohl Ranch Specific Plan site development guidelines. Landscaping will also be utilized as an integral part of the internal roadways as a traffic-calming element. <b>C</b>
Transportation	LU 12.6	Require that adequate and accessible circulation facilities exist to meet the demands of a proposed land use.	Implementation of mitigation measures described in the Traffic Section of this EIR section V.D.1. and updated measures provided in the EIR Addendum and conditions of approval for SP303A2 will ensure that demands of the project on circulation will be met. <b>C</b>
Utilities	OS 1.1	Balance consideration of water supply requirements among urban, agricultural, and environmental needs so that sufficient supply is available to meet each of these different needs.	A Water Supply Assessment has been prepared which determined that the Coachella Valley Water District will have adequate water to supply the proposed project and its various uses. <b>C</b>
Utilities	OS 2.1	Encourage the installation and use of water conserving systems such as dry wells and graywater systems, where feasible, in new developments. The installation of cisterns or infiltrators shall be encouraged to capture rainwater from roofs for irrigation in the dry season and flood control during heavy storms.	This project will utilize a "dual water system" consisting of potable and non-potable water systems. The potable system will provide water for domestic purposes and provide water for fire protection. The non-potable system will provide water for irrigation purposes. <b>C</b>
Utilities	OS 2.3	Encourage the use of native, drought-resistant landscaping planting.	The Kohl Ranch Specific Plan Landscape Guidelines require the implementation of water use efficiency measures such as use of drought tolerant plants, grouping plants with similar irrigation requirements, use of mulch, and water efficient irrigation systems, consistent with County Ordinance 859, Coachella Valley Water District Landscape Ordinance 1302.1 and the Thermal Design Guidelines plant palette. Additionally, the Kohl Ranch Specific Plan. <b>C</b>
Utilities	OS 2.4	Support and engage in educational outreach programs with other agencies that promote water conservation and widespread use of water-saving technologies.	The Kohl Ranch Specific Plan requires that a homeowners association shall be established for the specific plan area. The homeowners association will enforce any property and landscaping maintenance requirements established by the association and as appropriate will provide homeowners with information regarding maintenance methods. Ongoing educational programs including property maintenance methods are a part of The Kohl Ranch Community. <b>C</b>
Utilities	OS 4.5	Retain storm water at or near the site of generation for percolation into the groundwater to conserve it for future uses and to mitigate adjacent flooding.	The project, as indicated on the Drainage Plan of the Specific Plan, on-site runoff will be intercepted and conveyed through the development by means of a conventional catch basin, swales and storm drain system, in accordance with Coachella Valley Water District standards, so that the increase in on-site runoff resulting from the development will be detained on-site and allowed to percolate into the ground or be captured and reused. <b>C</b>
	ECVAP 9.1	Conserve and protect watersheds and water supply through adherence to policies contained in the Open Space, Habitat and Natural Resource Preservation and Land Use Designation Policies sections found in the General Plan Land Use Element, and the Water Resources section of the General Plan Multipurpose Open Space Element.	This project will utilize a "dual water system" consisting of potable and non-potable water systems. The potable system will provide water for domestic purposes and provide water for fire protection. The non-potable system will provide water for irrigation purposes. Additionally, the Kohl Ranch Specific Plan

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			Landscape Guidelines require the implementation of water use efficiency measures such as use of drought tolerant plants, grouping plants with similar irrigation requirements, use of mulch, and water efficient irrigation systems and are consistent with County Ordinance 859, Coachella Valley Water District Landscape Ordinance 1302.1 and the Thermal Design Guidelines plant palette. <b>C</b>
Utilities	S 4.10	Require all proposed projects anywhere in the County to address and mitigate any adverse impacts that it may have on the carrying capacity of local and regional storm drain systems.	The project, as shown on the Drainage section of the Specific Plan, storm flows will be intercepted and conveyed through the project area through utilization of graded swales and drainage pipe where they are discharged over weirs. Additionally, the use of on-site retention basins will control runoff flows. Section V.C.5, V.C.7 and V.D.2 analyze and incorporate mitigation to reduce impacts on carrying capacity of local and regional storm drains to less than significant. <b>C</b>
Utilities	LU 5.3	Review all Projects for consistency with individual urban water management plans.	A Water Supply Assessment has been prepared which considered the Coachella Valley Water District's Urban Water Management Plan and determined that the District has adequate water to supply to serve the proposed The Kohl Ranch Specific Plan. <b>C</b>
Utilities	LU 17.2	Require that adequate and available circulation facilities, water resources, sewer facilities and/or septic capacity exist to meet the demands of the proposed land use.	At the present time there are not adequate facilities available to meet all the needs of The Kohl Ranch. However, through County regulations and mitigation measures, which include the payment of development impact fees, and the construction of utility facilities, facilities can be constructed to meet the needs of the proposed project. A Water Supply Assessment has been prepared for this project and indicates that water resources are available to serve this project. <b>C</b>
Utilities	OS 2.2	Where feasible, decrease stormwater runoff by reducing pavement in development areas, and by design practices such as permeable parking bays and porous parking lots with bermed storage areas for rainwater detention.	The project, as shown on the Drainage section of the Specific Plan, storm flows will be intercepted and conveyed through the project area through utilization of graded swales and drainage pipe where they are discharged over weirs. Additionally, the use of on-site retention basins will control runoff flows. <b>C</b>
Utilities	LU 5.2	Monitor the capabilities of infrastructure and services in coordination with service providers, utilities, and outside agencies and jurisdictions to ensure that growth does not exceed acceptable levels of services.	The project proponent and utility providers will monitor growth and utility demand to ensure that the utility agency maintains adequate levels of service to the area. The payment of Riverside County's required development impact fees (Ord. No. 659) and the implementation of mitigation measures listed in the EIR shall ensure that growth is provided with appropriate infrastructures for adequate level of services. <b>C</b>
Utilities	LU 5.4	Ensure that development and conservation land uses do not infringe upon existing public utility corridors, including free owned rights-of-way and permanent easements, whose true land use is that of "public facilities". This policy will ensure that the "public facilities" designation governs over what otherwise may be inferred by the large scale general plan maps.	The Kohl Ranch designates a Public Facilities planning area and will ensure that land uses do not infringe on existing public utility corridors, such as the Avenue 64 Evacuation Channel that traverses through the project site. <b>C</b>
Utilities	OS 11.3	Permit and encourage the use of passive solar devices and other state-of- the-art energy resources.	The California Green Building Standards Code (proposed Part 11, Title 24) was adopted as part of the California Building Standards Code in the CCR. Part 11 establishes voluntary standards, that will become mandatory in the 2010 edition of the Code, on planning and design for sustainable site development, energy efficiency (in excess of the California Energy Code requirements), water conservation, material conservation, and internal air contaminants. The Project will be subject to these mandatory standards. <b>C</b>
Utilities	OS 16.1	Continue to implement Title 24 of the State Building Code. Establish mechanisms and incentives to encourage architects and builders to exceed the energy efficiency standards of Title 24.	The Greenhouse Gas section of the Addendum incorporates mitigation to continue implementation of Title 24 but with an exceedence of standards by 15%. <b>C</b>
Utilities	OS 16.2	Specify energy efficient materials and systems, including shade design technologies, for County buildings.	Although it not specifically planned at this point, any potential future County buildings will ensure compliance with this policy. <b>C</b>

Section	General Plan Policy No.	General Plan Policies	Relationship of the Project to the Policy
Utilities	OS 16.3	Implement public transportation systems that utilize alternative fuels when possible, as well as associated urban design measures that support alternatives to private automobile use.	EIR section V.D.1 incorporates mitigation to provide for potential bus transit. The project includes a trail system. This system will include a combination of greenbelts with linkages and sidewalks located adjacent to roads. It will incorporate trails for pedestrians, bicyclists, and equestrians in the form of an integrated system of hard- and soft-surface trails. The trail system will promote non-vehicular access to on-site recreational areas as well as to the schools. <b>C</b>
Utilities	OS 16.5	Utilize federal, state, and utility company programs that encourage energy conservation.	The Greenhouse Gas section of the Addendum incorporates mitigation to continue implementation of Title 24 with an exceedence of standards by 15%. Additionally, EIR section V.C.6 incorporates mitigation to promote usage of energy saving equipment. <b>C</b>
Utilities	OS 16.7	Promote purchasing of energy-efficient equipment based on a fair return on investment, and use energy-savings estimates as one basis for purchasing decisions for major energy-using devices.	EIR section V.C.6 incorporates mitigation to promote usage of energy saving equipment. <b>C</b>
Utilities	OS 16.8	Promote coordination of new public facilities with mass transit service and other alternative transportation services, including bicycles, and design structures to enhance mass transit, bicycle, and pedestrian use.	EIR section V.D.1 incorporates mitigation to provide for potential bus transit. The project includes a trail system. This system will include a combination of greenbelts with linkages and sidewalks located adjacent to roads. It will incorporate trails for pedestrians, bicyclists, and equestrians in the form of an integrated system of hard- and soft-surface trails. The trail system will promote non-vehicular access to on-site recreational areas as well as to the schools. <b>C</b>
Utilities	OS 16.9	Encourage increased use of passive, solar design and day-lighting in existing and new structures.	The Addendum incorporates mitigation to improve Title 24 standards by 15%. While not specifically required in the Kohl Ranch Specific Plan, use of passive, solar design and day-lighting in new structures could help reach this goal. <b>C</b>
Utilities	AQ 5.1	Utilize source reduction, recycling and other appropriate measures to reduce the amount of solid waste disposed of in landfills.	The project will participate in the County's recycling program. The Greenhouse Gas section of the Addendum incorporates mitigation that will require separate recycling and waste receptacles at each house and commercial site. <b>C</b>



# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

## STAFF REPORT

### ADMINISTRATIVE ITEMS

#### 4.1 Director's Approvals.

- A. During the period of October 15, 2017 through November 14, 2017, as authorized pursuant to Section 1.5.2(d) of the 2004 Riverside County Airport Land Use Compatibility Plan, ALUC Director Simon Housman reviewed one non-legislative case within Zone E of the Palm Springs International Airport Influence Area, two non-legislative cases within Zone D of the March Air Reserve Base/Inland Port Airport Influence Area, and one non-legislative cases within Zone E of the March Air Reserve Base/Inland Port Airport Influence Area and issued determinations of consistency.

ZAP1058PS17 (Palm Springs, Zone E) pertains to City of Cathedral City Case Nos. PUD 17-001 (Planned Unit Development) and TTM37354 (Tentative Tract Map No. 37354) proposing to divide 7.46 acres located northerly of Jones Road, easterly of Cree Road, and southerly of Carey Road into 48 single-family residential lots, plus additional lots for recreational, stormwater detention/retention, and open space purposes. The site is located within Compatibility Zone E, where residential density is not restricted. The elevation of Runway 13R-31L at Palm Springs International Airport at its southerly terminus is approximately 395.5 feet above mean sea level (395.5 feet AMSL). At a distance of approximately 10,670 feet from the runway to the project property line, structures with a top point elevation exceeding 502.2 feet AMSL would require notification to the Federal Aviation Administration Obstruction Evaluation Service (FAA OES). The proposed pads have an elevation of 324 feet AMSL, and the tallest proposed structure height is 26 feet, resulting in a maximum top point elevation of 350 feet AMSL, which is lower than the runway elevation. Therefore, FAA OES review for height/elevation reasons was not required. ALUC Director Simon Housman issued a determination of consistency for this project on October 26, 2017.

ZAP1287MA17 (March, Zone D) pertains to City of Moreno Valley Case No. PEN17-0001 (Plot Plan), a proposal to construct a 340,178 square foot industrial warehouse building on approximately 14.89 gross acres located on the southwest corner of Perris Boulevard and Nandina Avenue. The site is located within Compatibility Zone D of the March Air Reserve Base/Inland Port Airport Influence Area, where nonresidential intensity is not restricted. The elevation of Runway 14-32 at March Air Reserve Base/Inland Port Airport at its southerly terminus is approximately 1,488 feet above mean sea level (AMSL). At a distance of approximately 5,610 feet from the runway to the project property line, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with top of roof exceeding 1,544.7 feet AMSL. The proposed building has a finished floor elevation of 1,468.93 feet AMSL and a maximum height of 50 feet, resulting in a top point elevation of 1,518.93 feet AMSL. Therefore, FAA OES review for height/elevation reasons was not required. ALUC Director Simon Housman issued a determination of consistency for this project on October 26, 2017.

ZAP1290MA17 (March, Zone E) pertains to City of Riverside Case No. P17-0078 (Conditional Use Permit), a proposal to establish a church facility within an existing 5,400 square foot commercial building located at 3330 Durahart Street (on the east side of Durahart Street, southerly of 3<sup>rd</sup> Street and westerly of Chicago Avenue). The site is located within Compatibility Zone E of the March Air Reserve Base/Inland Port

Airport Influence Area, where nonresidential intensity is not restricted. The elevation of the site is more than 500 feet lower than the elevation of March's Runway 14-32 at its northerly terminus (approximately 1,535 feet above mean sea level [AMSL]). The site is located more than 20,000 feet from the runways at Riverside Municipal Airport. The site is located 16,725 feet from the northeasterly terminus of the runway at Flabob Airport, but the notice radius for that airport is 10,000 feet, since the runway length does not exceed 3,200 feet. Therefore, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review for height/elevation reasons was not required. ALUC Director Simon Housman issued a determination of consistency for this project on November 2, 2017.

ZAP1292MA17 (March, Zone D) pertains to City of Moreno Valley Case No. PEN17-0114 (Site Plan Review), a proposal to establish a truck storage facility with 93 truck/trailer stalls (involving conversion of an existing 3,034 square foot structure constructed as a four-bedroom home into an office) on 4.89 acres with an address of 24811 Rivard Road, on the southerly side of Rivard Road, westerly of Perris Boulevard. The site is located within Compatibility Zone D of the March Air Reserve Base/Inland Port Airport Influence Area, where nonresidential intensity is not restricted. The elevation of Runway 14-32 at March Air Reserve Base/Inland Port Airport at its southerly terminus is approximately 1,488 feet above mean sea level (AMSL). At a distance of approximately 5,962 feet from the runway to the project property line, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with top of roof exceeding 1,557 feet AMSL. The site elevation is 1,474 feet AMSL. The tallest new structure would be a 20 foot parking lot light standard, resulting in a top point elevation of 1,494 feet AMSL. Therefore, FAA OES review for height/elevation reasons was not required. ALUC Director Simon Housman issued a determination of consistency for this project on November 9, 2017.

B. Additionally, ALUC Director Simon Housman reviewed the following project pursuant to a specific delegation of authority issued by the Commission at its November 9, 2017 meeting.

ZAP1291MA17 (March, Zone C2) pertains to City of Riverside Case No. P17-0584 (Design Review), a proposal to construct a gated access parking lot with 114 parking stalls on 2.38 acres located along the southerly side of Lindbergh Drive/Grumman Drive, at the southerly terminus of Earhart Way, within the portion of the Mission Grove neighborhood located southerly of Mission Grove Parkway and easterly of Trautwein Road. The site is located within Compatibility Zone C2 of the March Air Reserve Base/Inland Port Airport Influence Area ("March AIA.") Within Compatibility Zone C2 of the March AIA, non-residential intensity is restricted to an average of 200 persons per acre and a maximum of 500 persons in any given single acre. Based on the number of parking spaces, the anticipated occupancy of the parking lot would be less than 100 persons per acre. The elevation of Runway 14-32 at its northerly terminus is approximately 1,535 feet above mean sea level (AMSL). At a distance of approximately 16,500 feet from the runway to the project site, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures exceeding a top point elevation of 1,700 feet AMSL. The project site has an elevation of 1,606 feet AMSL. The proposed parking lot light poles will be 20 feet in height, resulting in a top point elevation of 1,626 feet AMSL. Therefore, FAA OES review for height/elevation reasons was not required. ALUC Director Simon Housman issued a determination of consistency for this project on November 9, 2017.

- C. Additionally, ALUC Director Simon Housman reviewed the following jurisdiction-initiated non-impact legislative case pursuant to ALUC Resolution No. 2011-02 and issued a determination of consistency.

ZAP1025RG17 (Citywide – March and Perris Valley AIAs) pertains to City of Perris Case No. 17-05214 (Ordinance Amendment), a proposal to amend Chapter 19.69 of the City’s Zoning Code (a portion of the Perris Municipal Code), which addresses Parking and Loading Standards. Specifically, the proposed amendment updates required parking ratios for multi-family housing (apartments and attached residential units), raising the number of required parking spaces from 1.0 to 1.5 spaces per unit for studio and one-bedroom units and from 1.5 to 2.0 spaces per unit for two-bedroom units. 2.5 spaces per unit would be required for units with three or more bedrooms. Additionally, one guest parking space would be required per five dwelling units. For airport land use compatibility purposes, the density of residential development is evaluated on the basis of the numbers of dwelling units per acre, not on the basis of the number of parking spaces. As this amendment does not in itself increase density of residential uses or introduce new uses within any zoning classification, this amendment has no impact on the safety of air navigation within airport influence areas located within the City of Perris. ALUC Director Simon Housman issued a determination of consistency for this project on November 9, 2017.

- D. Additionally, ALUC Director Simon Housman reviewed the following legislative cases (along with associated non-legislative cases) within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area pursuant to ALUC Resolution No. 2015-01 and issued determinations of consistency.

ZAP1285MA17 (March, Zone E) pertains to City of Menifee Case Nos. SP 2017-187 (Specific Plan), a proposal to establish a new “Fleming Ranch” Specific Plan providing for up to 1,080 dwelling units on 222.5 acres, 20.4 acres of freeway-oriented commercial development, 12.9 acres of open space, and 36.6 acres of roadways within a 331-acre site located easterly of Encanto Drive and Interstate 215, southerly of Rouse Road, and westerly of Antelope Road, CZ 2017-188 (Zone Change), a proposal to change the zoning classification of the site from One-family Dwellings (R-1) and Scenic Highway Commercial (C-P-S) to Specific Plan, and 2017-264 (Tentative Tract Map No. 37391), a proposal to divide the 331-acre site into 17 lots largely corresponding to the individual residential Planning Areas of the Specific Plan. The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA), where residential density is not restricted. Although the project site is located within the March Air Reserve Base/Inland Port AIA, the actual nearest runway is Runway 15-33 at Perris Valley Airport. The southerly terminus of this runway is located approximately 14,000 feet from the project site. At this distance, given the runway elevation of 1,415 feet above mean sea level (AMSL), Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with top of roof exceeding 1,555 feet AMSL. The site has an existing maximum elevation of approximately 1,430 feet AMSL. With a maximum structure height of 45 feet, the top point elevation would be 1,475 feet AMSL, which is lower than the runway elevation at March Air Reserve Base/Inland Port Airport (1,488 feet AMSL at its southerly terminus). Therefore, FAA OES review for height/elevation reasons was not required. ALUC Director Simon Housman issued a determination of consistency for this project on October 26, 2017.

ZAP1288MA17 (March, Zone E) pertains to City of Moreno Valley Case Nos. PEN 17-0134 (Change of Zone), a proposal to change the zoning classification of 4.47 acres located southerly of John F. Kennedy Drive and westerly of Perris Boulevard from Neighborhood Commercial (NC) to Community Commercial (CC), and PEN 17-0135 (Conditional Use Permit), a proposal to construct a 90,511 square foot mini-storage facility on that property. The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA), where nonresidential intensity is not restricted. The elevation of Runway 14-32 at March Air Reserve Base/Inland Port Airport is approximately 1,535 feet above mean sea level (AMSL) at its northerly terminus and 1,488 feet AMSL at its southerly terminus. At a distance of 11,848 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review could be required for any structure with an elevation at top point exceeding 1,606 feet AMSL. The site's elevation is 1,536 feet AMSL. With a maximum building height of 16 feet, the top point elevation would be 1,552 feet AMSL. Therefore, FAA OES review for height/elevation reasons was not required. ALUC Director Simon Housman issued a determination of consistency for this project on October 26, 2017.

ZAP1289MA17 (March, Zone E) pertains to City of Riverside Case Nos. P17-0781 (Rezone), a proposal to change the zoning of seven parcels totaling 2.04 acres located on the northwest corner of Chicago Avenue and 7<sup>th</sup> Street from Single-Family Residential (R-1-7000) to Multiple-Family Residential (R-3-1500) and P17-0853 (General Plan Amendment), a proposal to amend the land use designation of four of these parcels from Medium Density Residential to High Density Residential. (There is no associated discretionary development application at this time.) The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA), where residential density and nonresidential intensity are not restricted. ALUC Director Simon Housman issued a determination of consistency for this project on November 9, 2017.

Copies of these consistency letters and background documents are attached, for the Commission's information.

Y:\ALUC\ALUC Administrative Items\ADmin Item 12-14-17 (Corrected).doc

**AIRPORT LAND USE COMMISSION  
RIVERSIDE COUNTY**



October 26, 2017

**CHAIR**  
**Rod Ballance**  
Riverside

Mr. Robert Rodriguez, Planning Manager  
City of Cathedral City Planning Department  
68-700 Avenida Lalo Guerrero  
Cathedral City, CA 92234

**VICE CHAIRMAN**  
**Steve Manos**  
Lake Elsinore

**COMMISSIONERS**

**Arthur Butler**  
Riverside

**John Lyon**  
Riverside

**Glen Holmes**  
Hemet

**Russell Betts**  
Desert Hot Springs

**Steven Stewart**  
Palm Springs

**STAFF**

**Director**  
**Simon A. Housman**

John Guerin  
Paul Rull  
Barbara Santos

County Administrative Center  
4080 Lemon St., 14<sup>th</sup> Floor  
Riverside, CA 92501  
(951) 955-5132

[www.rcaluc.org](http://www.rcaluc.org)

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –  
DIRECTOR'S DETERMINATION**

File No.: ZAP1058PS17  
Related File No.: PUD 17-001 (Planned Unit Development), TTM 37354  
(Tentative Tract Map)  
APNs: 681-310-014 and -016

Dear Mr. Rodriguez:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to ALUC's general delegation as per Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed City of Cathedral City Case Nos. PUD 17-001 (Planned Unit Development) and TTM 37354 (Tentative Tract Map) proposing to divide 7.46 acres located northerly of Jones Road, easterly of Cree Road, and southerly of Carey Road into 48 single-family residential lots, plus additional lots for recreational, stormwater detention/retention, and open space purposes.

The site is located within Airport Compatibility Zone E of the Palm Springs International Airport Influence Area (AIA). Within Compatibility Zone E of the Palm Springs International Airport Land Use Compatibility Plan, residential density is not restricted.

The elevation of Runway 13R-31L at Palm Springs International Airport at its southerly terminus is approximately 395.5 feet above mean sea level (395.5 feet AMSL). At a distance of approximately 10,670 feet from the runway to the project property line, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with top of roof exceeding 502.2 feet AMSL. The proposed pad elevation is 324 feet AMSL, and the tallest proposed structure height is 26 feet, resulting in a maximum top point elevation of 350 feet AMSL -below the runway elevation. Therefore, review by the FAA OES was not required.


As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2005 Palm Springs Airport Land Use Compatibility Plan, provided that the City of Cathedral City applies the following recommended conditions:

**CONDITIONS:**

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, outdoor production of cereal grains, sunflower, and row crops, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, and construction and demolition debris facilities.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached "Notice of Airport in Vicinity" shall be provided to all potential purchasers of the lots and to the tenants of the homes thereon.
4. Any new detention basins on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.

If you have any questions, please contact Paul Rull, ALUC Urban Regional Planner IV, at (951) 955-6893, or John Guerin, ALUC Principal Planner, at (951) 955-0982.

Sincerely,  
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Simon A. Housman, ALUC Director

Attachment: Notice of Airport in Vicinity

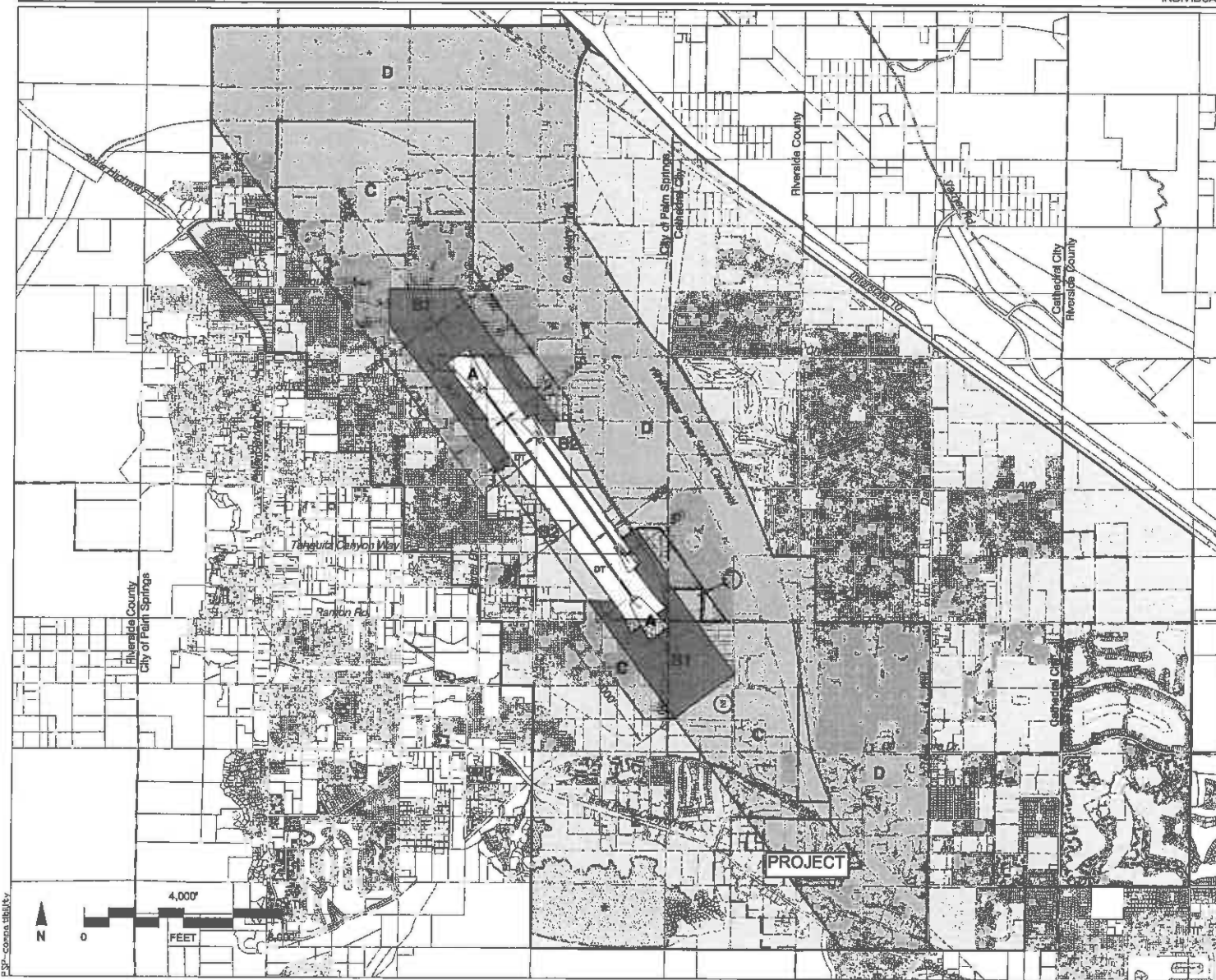
cc: Mario Gonzalez, GHA Enterprises, Inc. (applicant/representative)  
Bilt-Mor Development, LLC (stated as property owner)  
Pensco Trust Co. (listed property owner)  
Billy and Treva Waddle (fee-payer) (Cypress address)  
Mr. Thomas Nolan, Executive Director, Palm Springs International Airport  
ALUC Case File

Y:\AIRPORT CASE FILES\Palm Springs\ZAP1058PS17\ZAP1058PS17.LTR.doc

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)





**Legend**

- Compatibility Zones**
- Airport Influence Area Boundary
  - Zone A
  - Zone B1
  - Zone B2
  - Zone C
  - Zone D
  - Zone E
  - Height Review Overlay Zone

**Boundary Lines**

- Airport Property Line
- City Limits

**Notes**

All dimensions measured from runway ends and centerlines.

DT = Displaced Threshold

See Chapter 2, Table 2A for compatibility criteria associated with this map.

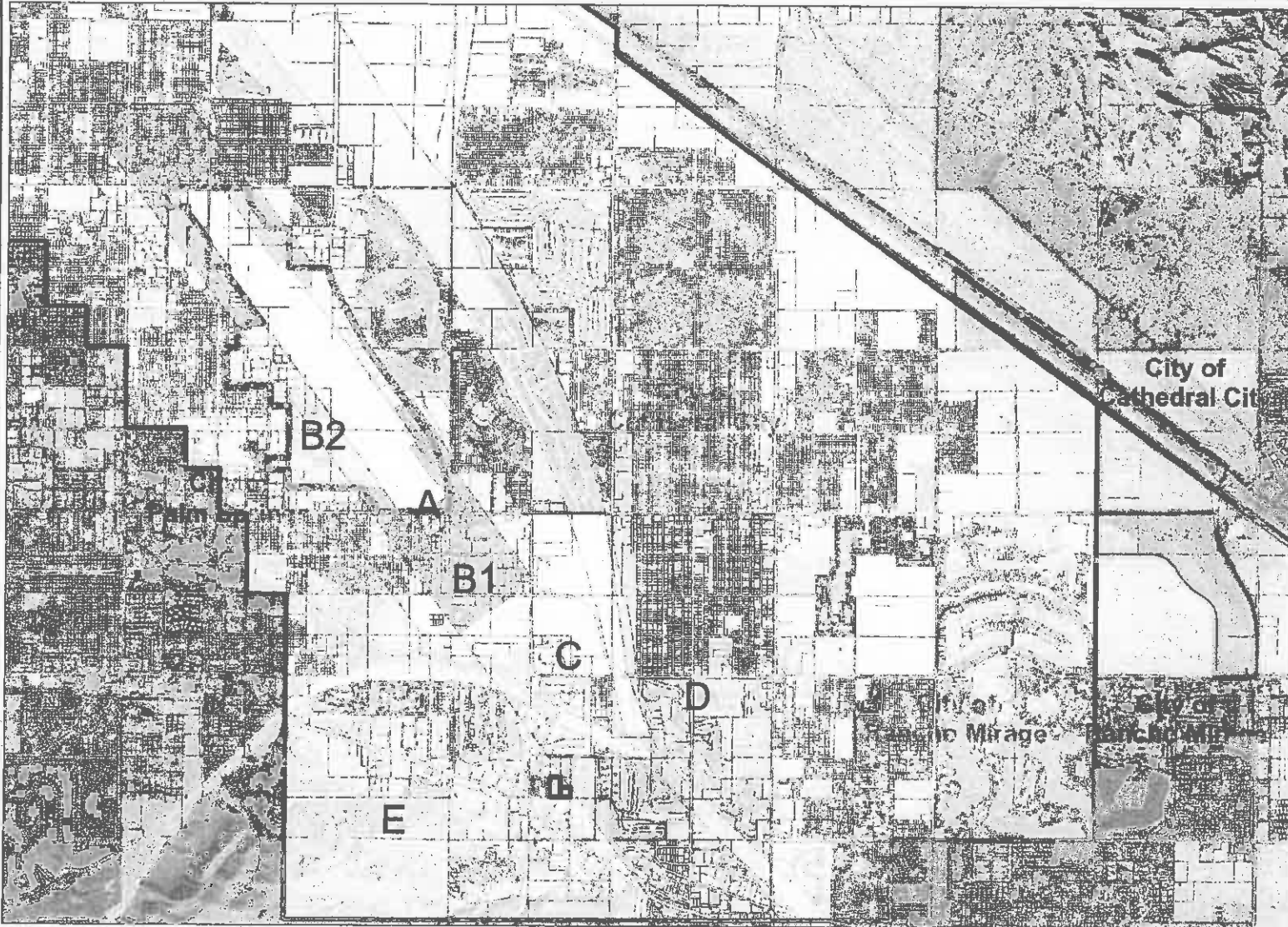
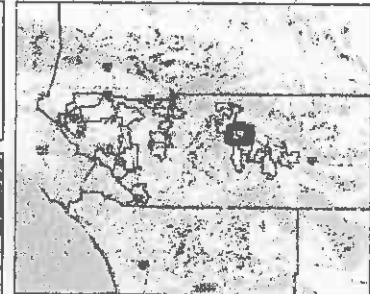
See Policy PS.2.1.

Riverside County  
 Airport Land Use Commission  
 Riverside County  
 Airport Land Use Compatibility Plan  
 Policy Document  
 (Adopted March 2005)

Map PS-1

**Compatibility Map**  
 Palm Springs International Airport

# My Map



**Legend**

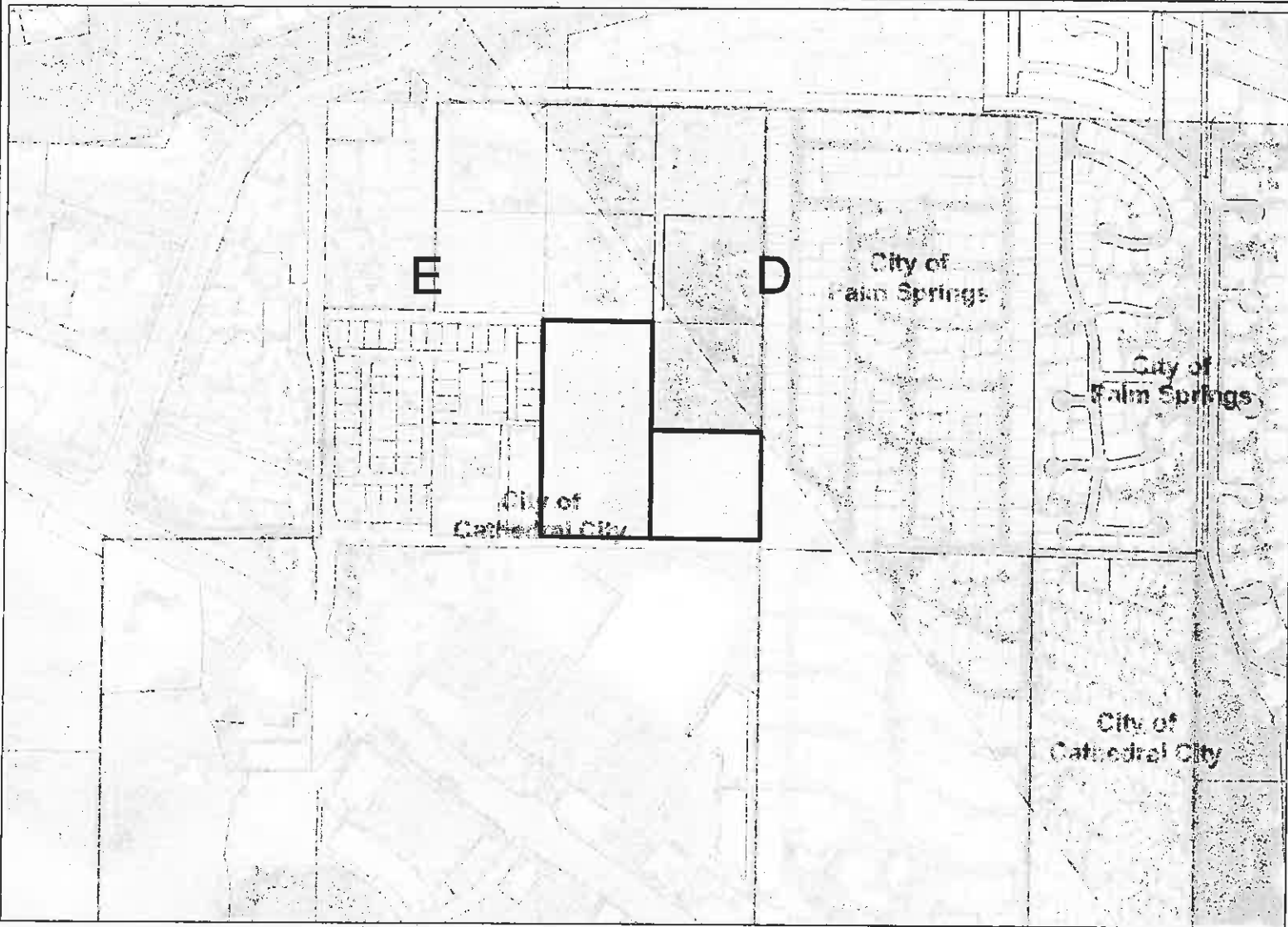
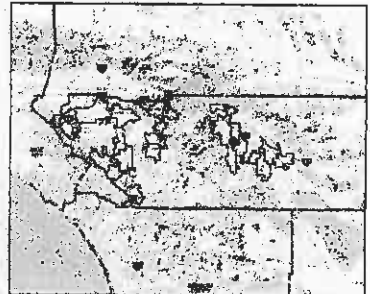
- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

**Notes**

# My Map



### Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
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- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



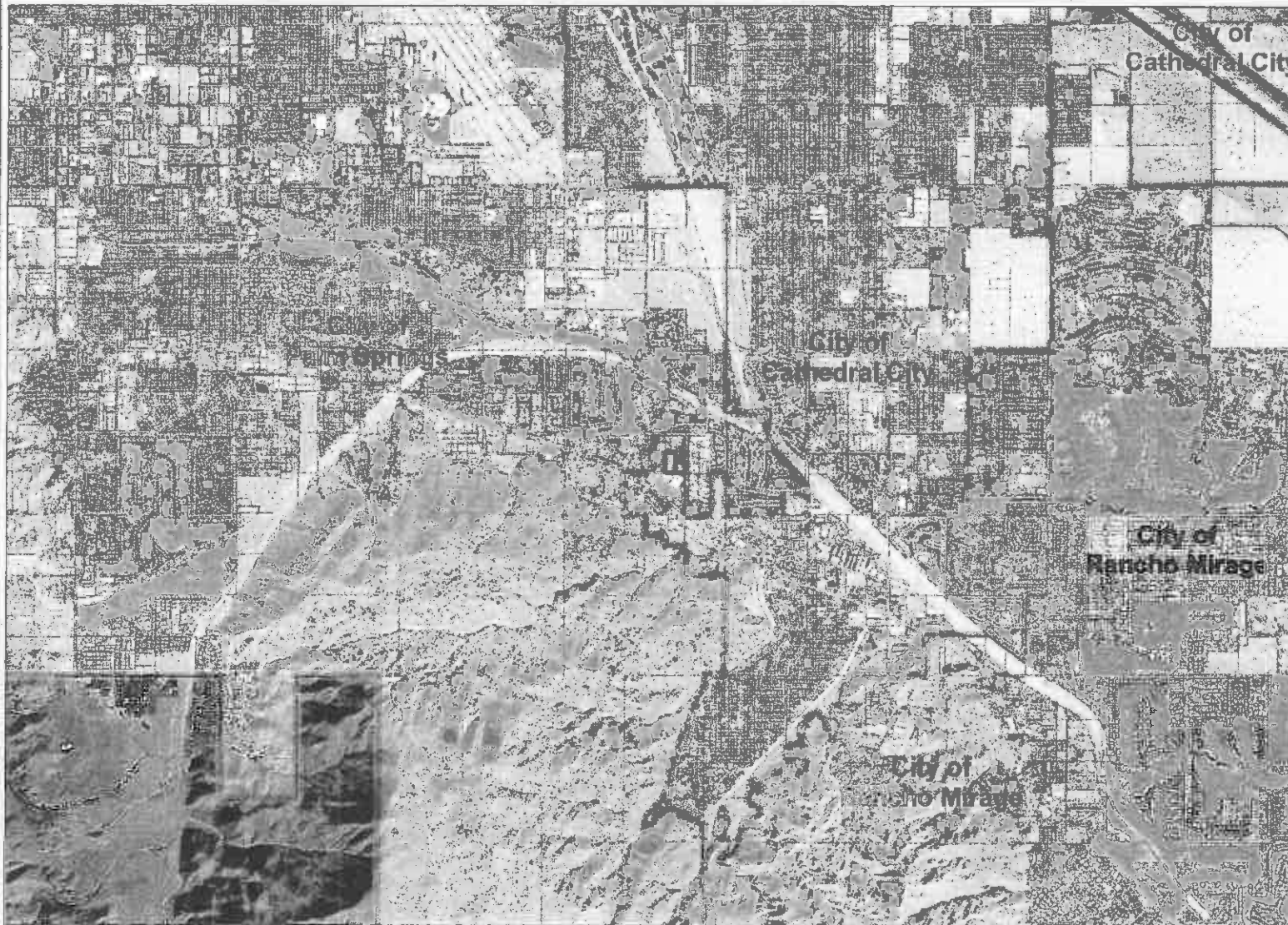
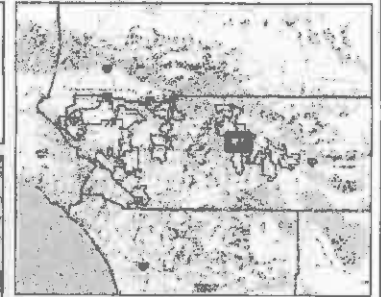
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### Notes

# My Map



## Legend

- City Boundaries
- Cities**
- highways\_large
  - HWY
  - INTERCHANGE
  - INTERSTATE
  - USHWY
- majorroads
- counties
- cities
- hydrographylines
- waterbodies
  - Lakes
  - Rivers



0 6,065 12,129 Feet



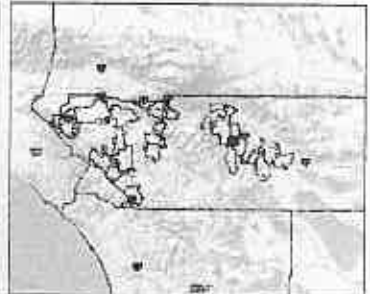
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## Notes

# My Map



## Legend

- City Boundaries
- Cities
- roads
- anno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 1,516 3,032 Feet



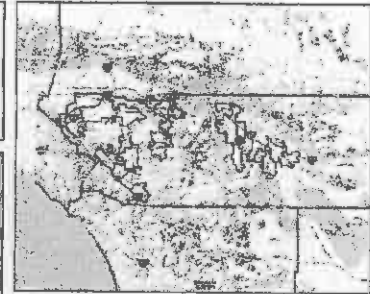
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## Notes

# My Map



## Legend

-  City Boundaries
-  Cities
-  roadsanno
-  highways
-  HWY
-  INTERCHANGE
-  INTERSTATE
-  OFFRAMP
-  ONRAMP
-  USHWY
-  counties
-  cities
-  hydrographylines
-  waterbodies
-  Lakes
-  Rivers



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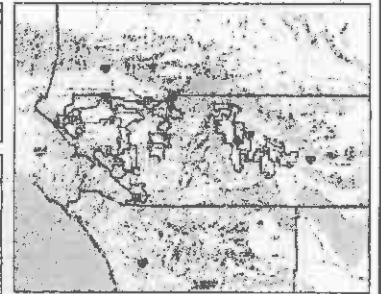
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## Notes

# My Map



## Legend

- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



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379

758 Feet



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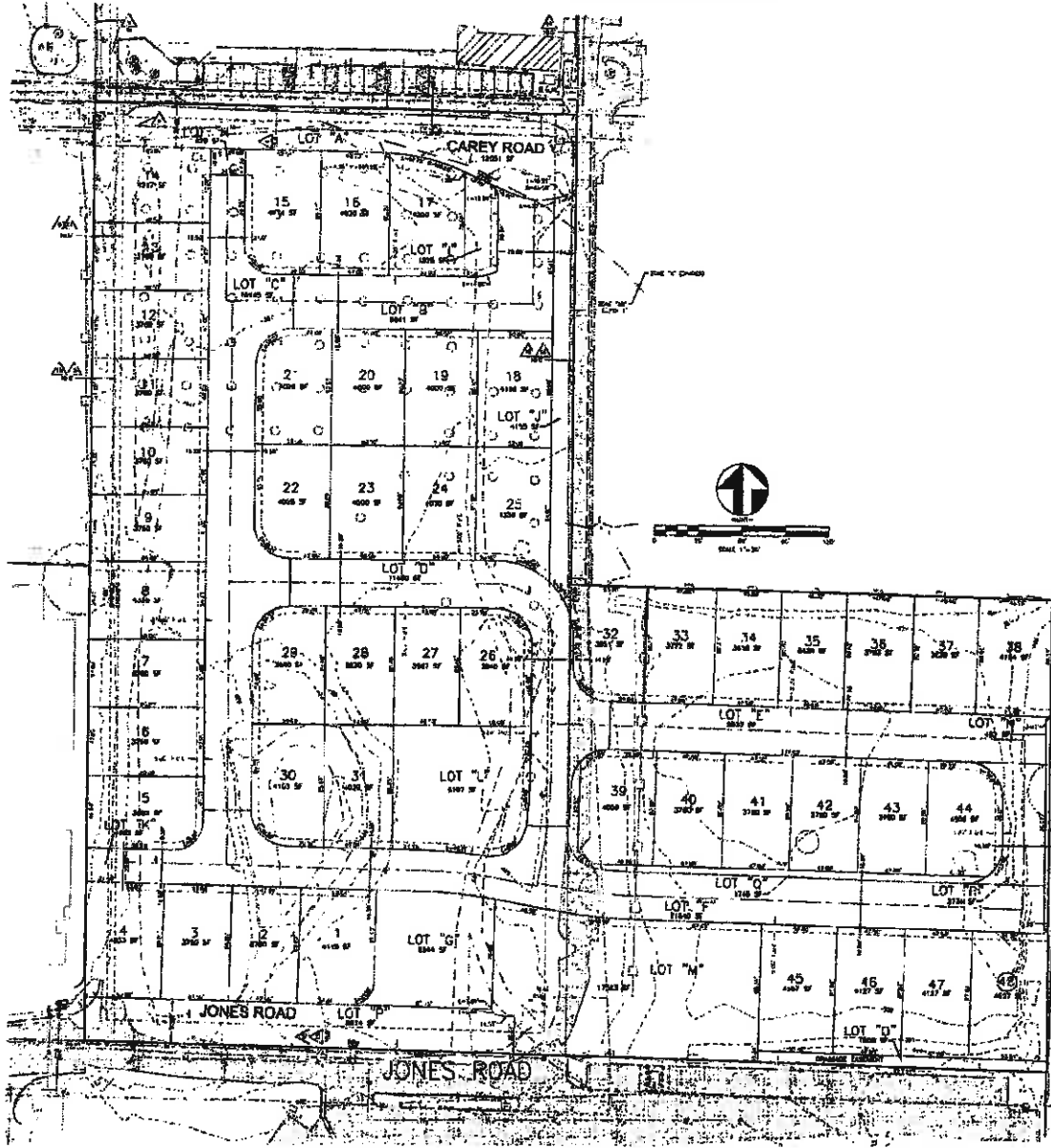
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## Notes

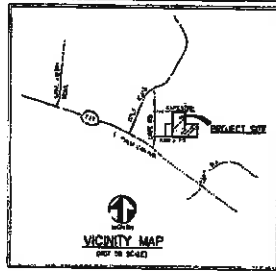
IN THE CITY OF CALIFORNIA, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA  
**TENTATIVE TRACT MAP NO. 37354**

APPROX. 150 TO 160 SQUARE FEET PER LOT TO BE BOUND BY THE WEST AND SOUTH SIDES OF THE  
 WEST COURSE, CALIFORNIA A.C. SEC. 26, T. 42N., R. 12E.



**LOT INFORMATION**

26	1500 SQ. FT.	1500 SQ. FT.
27	1500 SQ. FT.	1500 SQ. FT.
28	1500 SQ. FT.	1500 SQ. FT.
29	1500 SQ. FT.	1500 SQ. FT.
30	1500 SQ. FT.	1500 SQ. FT.
31	1500 SQ. FT.	1500 SQ. FT.
32	1500 SQ. FT.	1500 SQ. FT.
33	1500 SQ. FT.	1500 SQ. FT.
34	1500 SQ. FT.	1500 SQ. FT.
35	1500 SQ. FT.	1500 SQ. FT.
36	1500 SQ. FT.	1500 SQ. FT.
37	1500 SQ. FT.	1500 SQ. FT.
38	1500 SQ. FT.	1500 SQ. FT.
39	1500 SQ. FT.	1500 SQ. FT.
40	1500 SQ. FT.	1500 SQ. FT.
41	1500 SQ. FT.	1500 SQ. FT.
42	1500 SQ. FT.	1500 SQ. FT.
43	1500 SQ. FT.	1500 SQ. FT.
44	1500 SQ. FT.	1500 SQ. FT.
45	1500 SQ. FT.	1500 SQ. FT.
46	1500 SQ. FT.	1500 SQ. FT.
47	1500 SQ. FT.	1500 SQ. FT.



**APPROXIMATE**  
 APPROXIMATE ZONING  
 APPROXIMATE FLOOD ZONING  
 APPROXIMATE UTILITY PLANS  
 APPROXIMATE SOURCE OF WATER SUPPLY  
 APPROXIMATE METHOD OF WASTE DISPOSAL  
 APPROXIMATE SCHOOL DISTRICT  
 APPROXIMATE LOT AREA

**LEGAL DESCRIPTION**  
 APPROXIMATE LEGAL DESCRIPTION  
 APPROXIMATE SOURCE OF WATER SUPPLY  
 APPROXIMATE METHOD OF WASTE DISPOSAL  
 APPROXIMATE SCHOOL DISTRICT  
 APPROXIMATE LOT AREA

**ABBREVIATIONS**  
 APPROXIMATE LEGAL DESCRIPTION  
 APPROXIMATE SOURCE OF WATER SUPPLY  
 APPROXIMATE METHOD OF WASTE DISPOSAL  
 APPROXIMATE SCHOOL DISTRICT  
 APPROXIMATE LOT AREA

**LEGEND**  
 APPROXIMATE LEGAL DESCRIPTION  
 APPROXIMATE SOURCE OF WATER SUPPLY  
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 APPROXIMATE LOT AREA

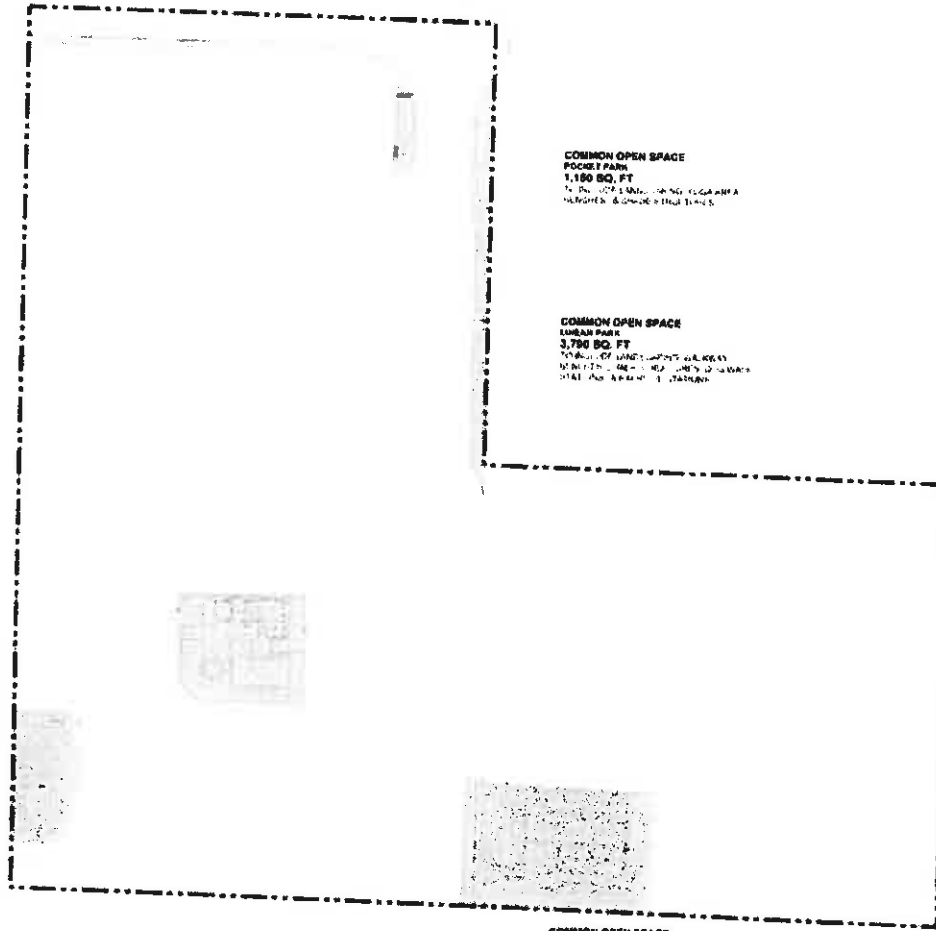
**APPROXIMATE**  
 APPROXIMATE ZONING  
 APPROXIMATE FLOOD ZONING  
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**LEGEND**  
 APPROXIMATE LEGAL DESCRIPTION  
 APPROXIMATE SOURCE OF WATER SUPPLY  
 APPROXIMATE METHOD OF WASTE DISPOSAL  
 APPROXIMATE SCHOOL DISTRICT  
 APPROXIMATE LOT AREA

**OWNER/APPLICANT**  
 PREPARED BY:  
 TENTATIVE TRACT MAP NO. 37354  
 SHEET 1 OF SHEET





**OPEN SPACE SUMMARY**

PARK / DETENTION AREA	12,790 SF
RECREATION AREA	7,900 SF
ENTRY PARK	4,500 SF
LINEAR PARK	3,790 SF
POCKET PARK	1,160 SF
<b>TOTAL</b>	<b>30,140 SF*</b>

\*CITY REQUIREMENT FOR COMMON OPEN SPACE IS 500 S.F. PER UNIT. WITH 48 TOTAL UNITS, THE REQUIREMENT FOR THIS PROJECT IS 24,000 SF. PROJECT EXCEEDS THIS REQUIREMENT BY 6,140 SF.

**COMMON OPEN SPACE  
RECREATION AREA  
7,900 SQ. FT.**  
TOTAL OF UNITS: 48 UNITS  
TOTAL AREA: 1,100 SQ. FT. (1,100 SQ. FT. MINIMUM)

**COMMON OPEN SPACE  
ENTRY PARK  
4,500 SQ. FT.**  
TOTAL OF UNITS: 48 UNITS  
TOTAL AREA: 1,100 SQ. FT. (1,100 SQ. FT. MINIMUM)

**COMMON OPEN SPACE  
POCKET PARK  
1,160 SQ. FT.**  
TOTAL OF UNITS: 48 UNITS  
TOTAL AREA: 1,100 SQ. FT. (1,100 SQ. FT. MINIMUM)

**COMMON OPEN SPACE  
LINEAR PARK  
3,790 SQ. FT.**  
TOTAL OF UNITS: 48 UNITS  
TOTAL AREA: 1,100 SQ. FT. (1,100 SQ. FT. MINIMUM)

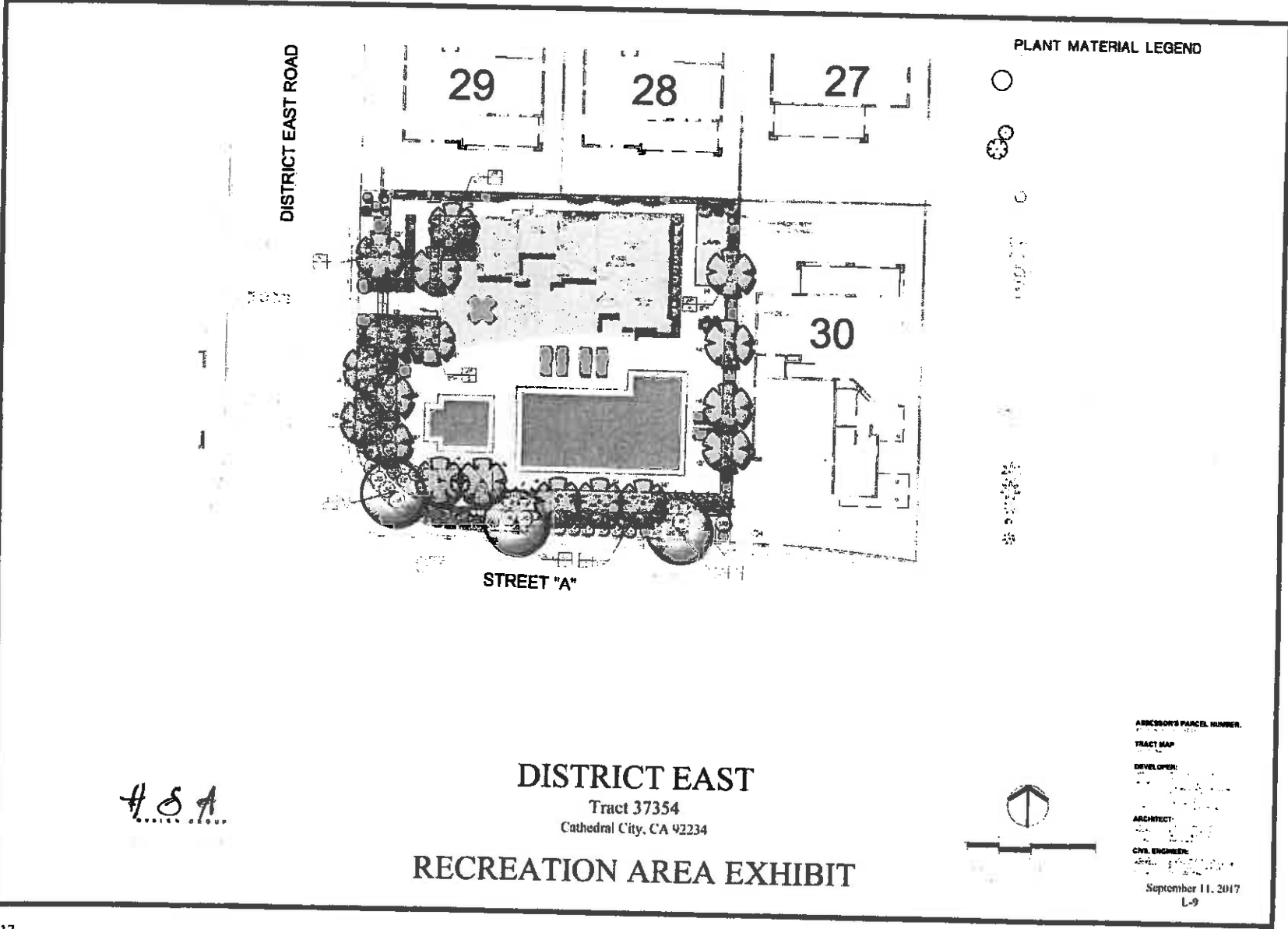
**COMMON OPEN SPACE  
PARK / DETENTION AREA  
12,790 SQ. FT.**  
TOTAL OF UNITS: 48 UNITS  
TOTAL AREA: 1,100 SQ. FT. (1,100 SQ. FT. MINIMUM)

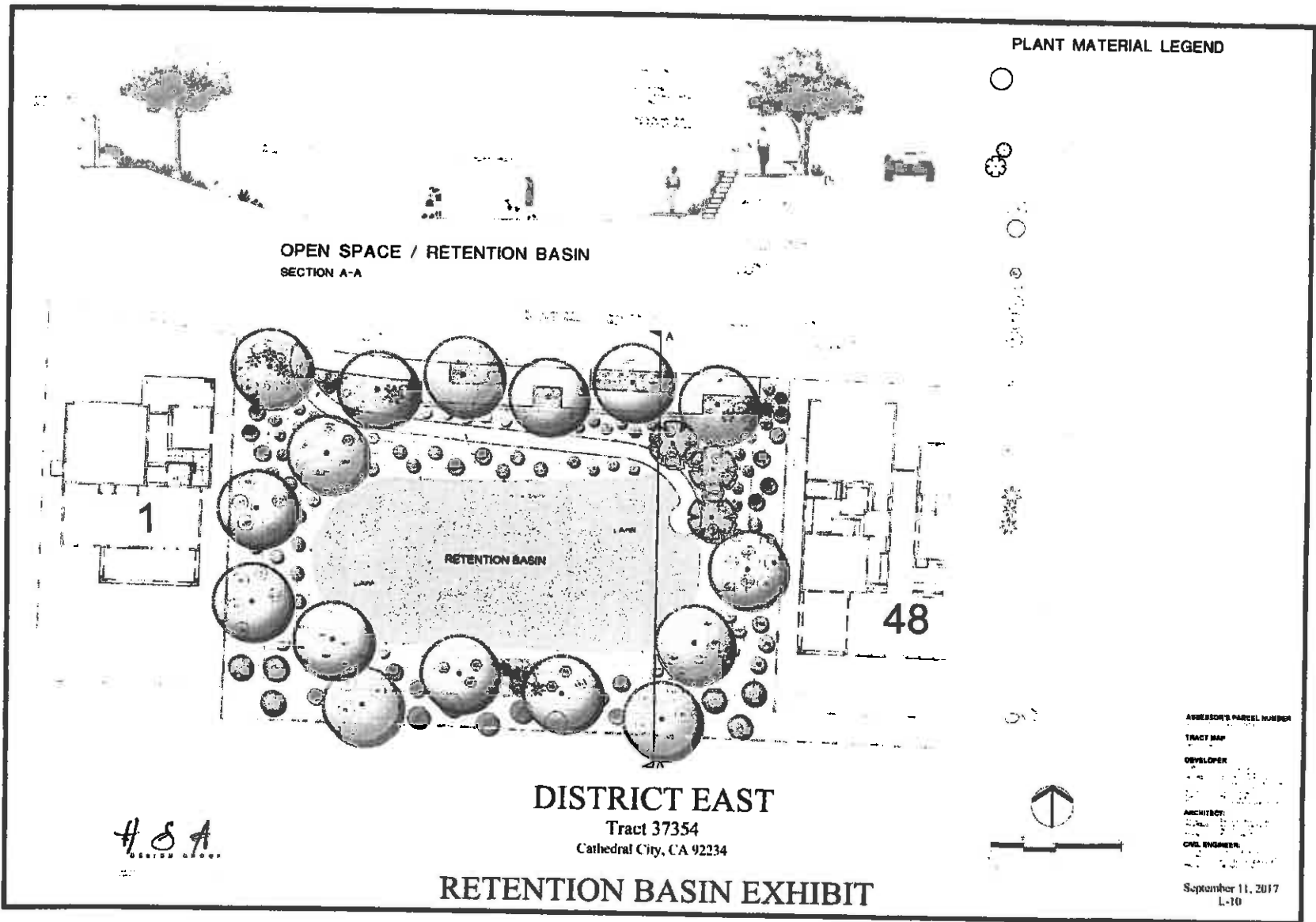
**BILTMORE DEVELOPMENT /  
GHA COMMUNITIES  
DISTRICT EAST**  
CATHEDRAL CITY, CA

APN: 681-310-014 and 681-310-016  
COMMON OPEN SPACE

Owner:  
BILTMORE DEVELOPMENTAL COMMUNITIES  
1000 10TH STREET, SUITE 100  
CATHEDRAL CITY, CA 92009  
TEL: 760-333-1111  
WWW.BILTMORECOMMUNITIES.COM

SEPTEMBER 11, 2013  
  
**DANIELIAN ASSOCIATES**  
 ARCHITECTURE • PLANNING  
 1000 10TH STREET, SUITE 100  
 CATHEDRAL CITY, CA 92009  
 TEL: 760-333-1111  
 WWW.DANIELIANASSOCIATES.COM  
**A-3**





**PLANT MATERIAL LEGEND**

**OPEN SPACE / RETENTION BASIN**  
SECTION A-A

**DISTRICT EAST**  
Tract 37354  
Cathedral City, CA 92234

**RETENTION BASIN EXHIBIT**

**HSA**  
DESIGN GROUP

ASSESSOR'S PARCEL NUMBER  
TRACT MAP  
DEVELOPER  
ARCHITECT  
CIVIL ENGINEER  
September 11, 2017  
L-10

PAGE BREAK



# AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY



October 26, 2017

Ms. Julia Descoteaux, Associate Planner  
City of Moreno Valley Community Development Department  
14177 Frederick Street  
Moreno Valley CA 92553

**CHAIR**  
Rod Ballance  
Riverside

**VICE CHAIRMAN**  
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Lake Elsinore

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Palm Springs

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Director  
Simon A. Housman

John Guerin  
Paul Rull  
Barbara Santos

County Administrative Center  
4080 Lemon St., 14th Floor  
Riverside, CA 92501  
(951) 955-5132

[www.rcaluc.org](http://www.rcaluc.org)

## RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW – DIRECTOR'S DETERMINATION

File No.: ZAP1287MA17  
Related File No.: PEN17-0001 (Plot Plan)  
APNs: 316-210-036 thru -038

Dear Ms. Descoteaux:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed City of Moreno Valley Case No. PEN17-0001 (Plot Plan), a proposal to construct a 340,178 square foot industrial warehouse building on approximately 14.89 gross acres located on the southwest corner of Perris Boulevard and Nandina Avenue.

The site is located within Airport Compatibility Zone D of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone D of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, nonresidential intensity is not restricted.

The elevation of Runway 14-32 at March Air Reserve Base/Inland Port Airport at its southerly terminus is approximately 1,488 feet above mean sea level (1488 AMSL). At a distance of approximately 5,610 feet from the runway to the project property line, Federal Aviation Administration Obstruction Evaluation Services (FAA OES) review would be required for any structures with top of roof exceeding 1,544.7 feet AMSL. The proposed building has a finished floor elevation of 1,468.93 feet AMSL and a maximum height of 50 feet, resulting in a top point elevation of 1,518.93 feet AMSL. Therefore, review by the FAA OES was not required.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, provided that the City of Moreno Valley applies the following recommended conditions:

### CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site.

## AIRPORT LAND USE COMMISSION

- (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached notice shall be provided to all potential purchasers of the property and to tenants of the building(s) thereon, and shall be recorded as a deed notice.
  4. The proposed detention basins on the site (including water quality management basins) shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
  5. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment of irrigation controllers, access gates, etc.

If you have any questions, please contact Paul Rull, ALUC Urban Regional Planner IV, at (951) 955-6893 or John Guerin, ALUC Principal Planner, at (951) 955-0982.

Sincerely,  
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



---

Simon A. Housman, ALUC Director

Attachments: Notice of Airport in Vicinity

## AIRPORT LAND USE COMMISSION

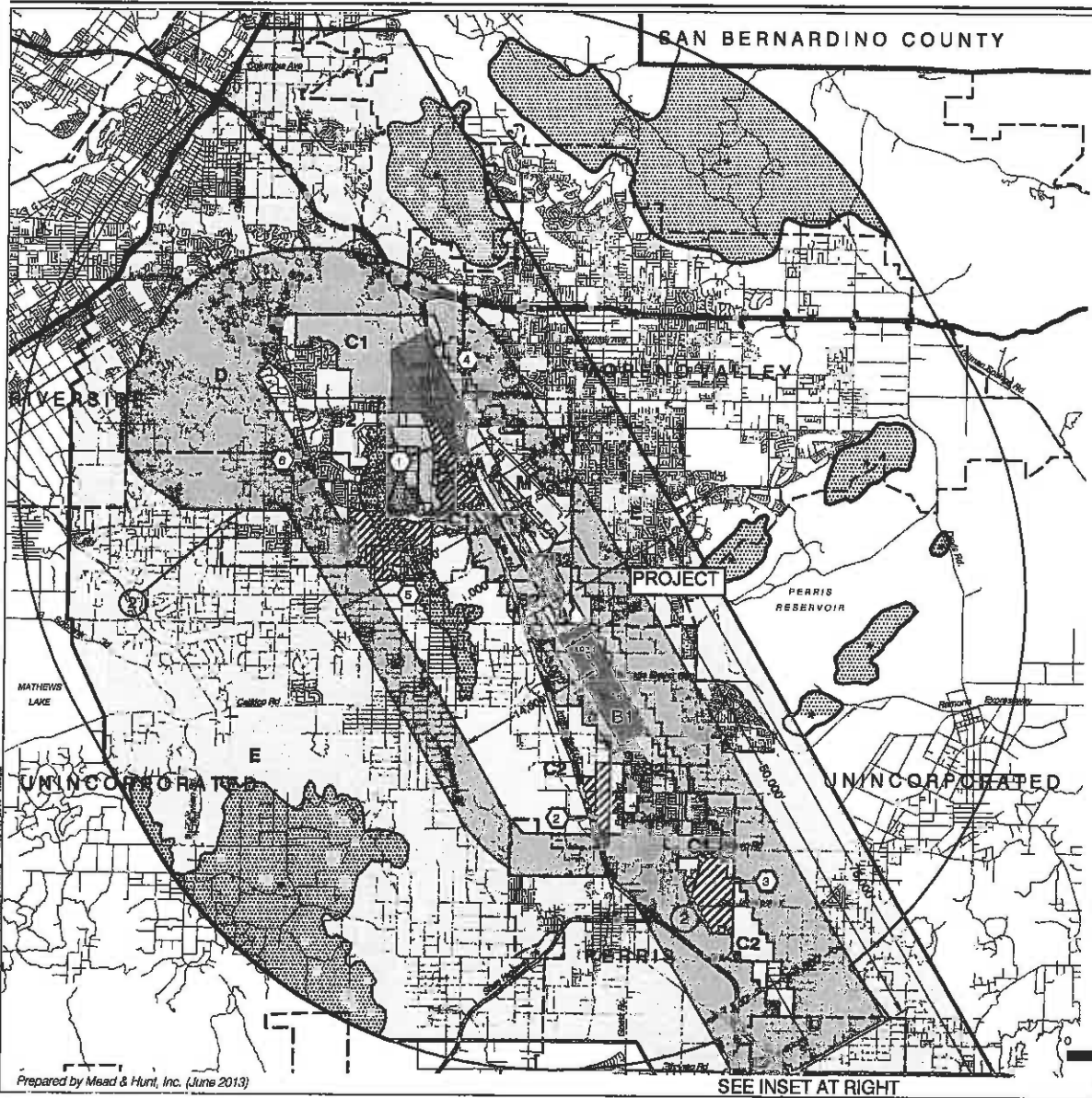
cc: Duke Realty, Adam Schmid (applicant)  
T&B Planning, George Atalla (representative)  
Newcastle Partners, Inc., Dennis Higgs (property owner)  
Kim Khue Thi Tran (Attn.: Nancy Trang) (property owner)  
Panayiotis and Adriana Katelaris (property owner)  
Duke Realty, Indianapolis (fee-payer/property owner)  
Gary Gosliga, Airport Manager, March Inland Port Airport Authority  
Denise Hauser or Daniel Rockholt, March Air Reserve Base  
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1287MA17\ZAP1287MA17.LTR.doc

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)





**LEGEND**

**Compatibility Zones**

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

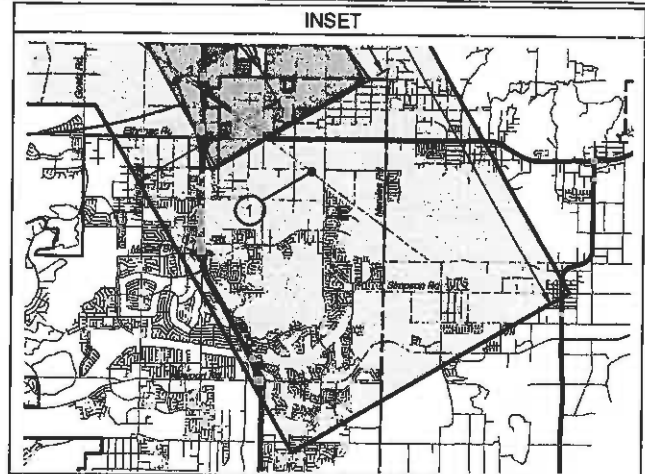
**Boundary Lines**

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- - - City Limits
- ▨ Site-Specific Exceptions (existing local agency commitments to development projects)

① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.

② Point at which departing aircraft typically reach 3,000 feet above runway end.

① March JPA: March Business Center/Meridian  
 ② Perris: Harvest Landing  
 ③ Perris: Park West  
 ④ Moreno Valley: Affordable Housing  
 ⑤ March JPA: Ben Clark Training Center  
 ⑥ Riverside: Ridge Crest Subdivision



Note:  
All dimensions are measured from runway ends and centerlines.



Base map source: County of Riverside 2013

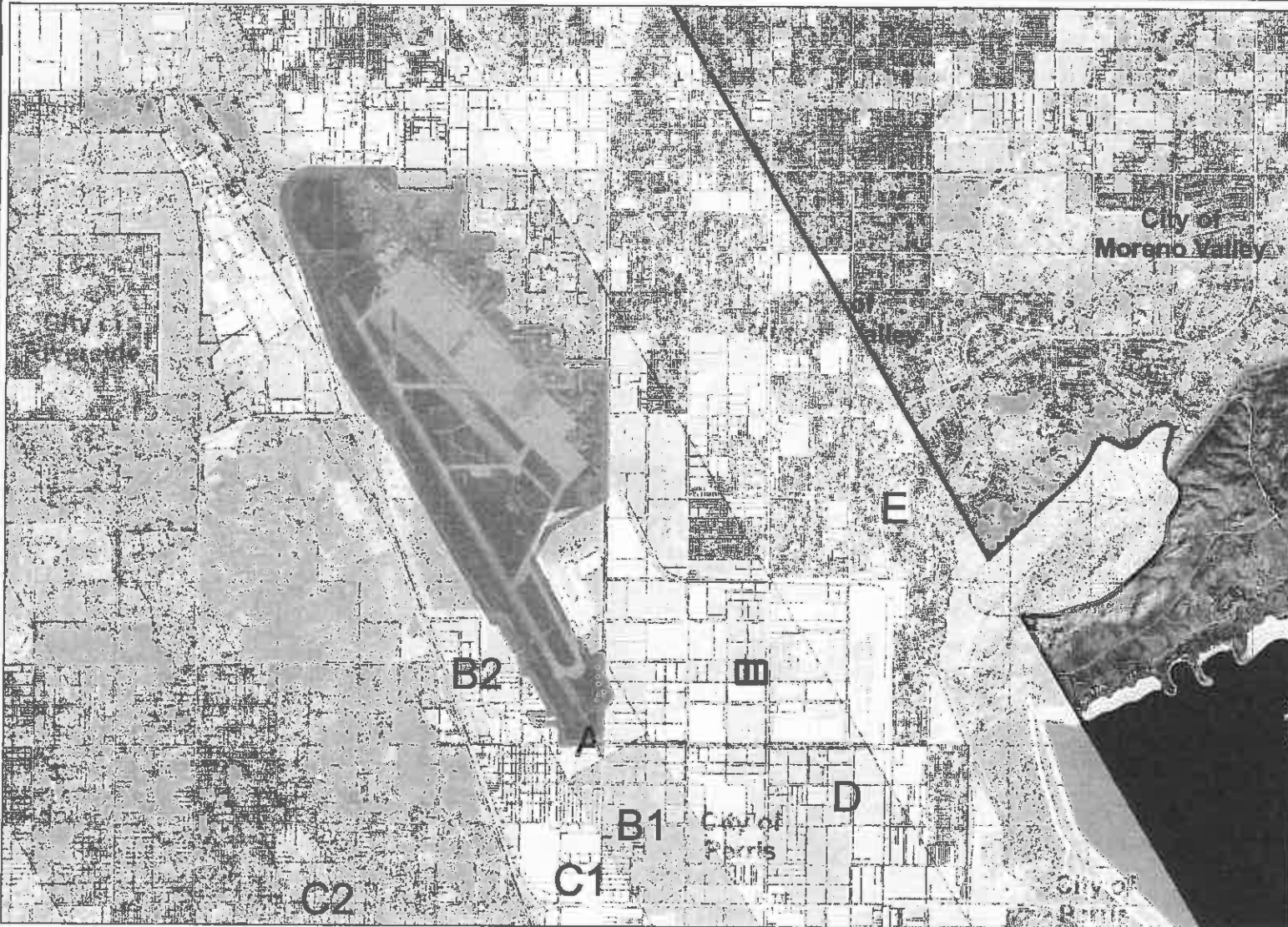
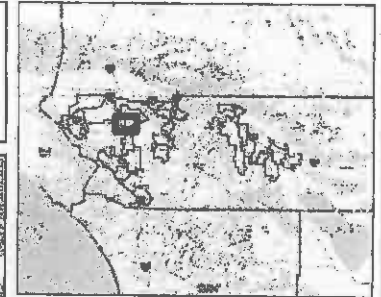
**Riverside County  
 Airport Land Use Commission  
 March Air Reserve Base / Inland Port Airport  
 Land Use Compatibility Plan  
 (Adopted November 13, 2014)**

Map MA-1  
**Compatibility Map**  
 March Air Reserve Base / Inland Port Airport

Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

# My Map



## Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-FXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



0 6,065 12,129 Feet



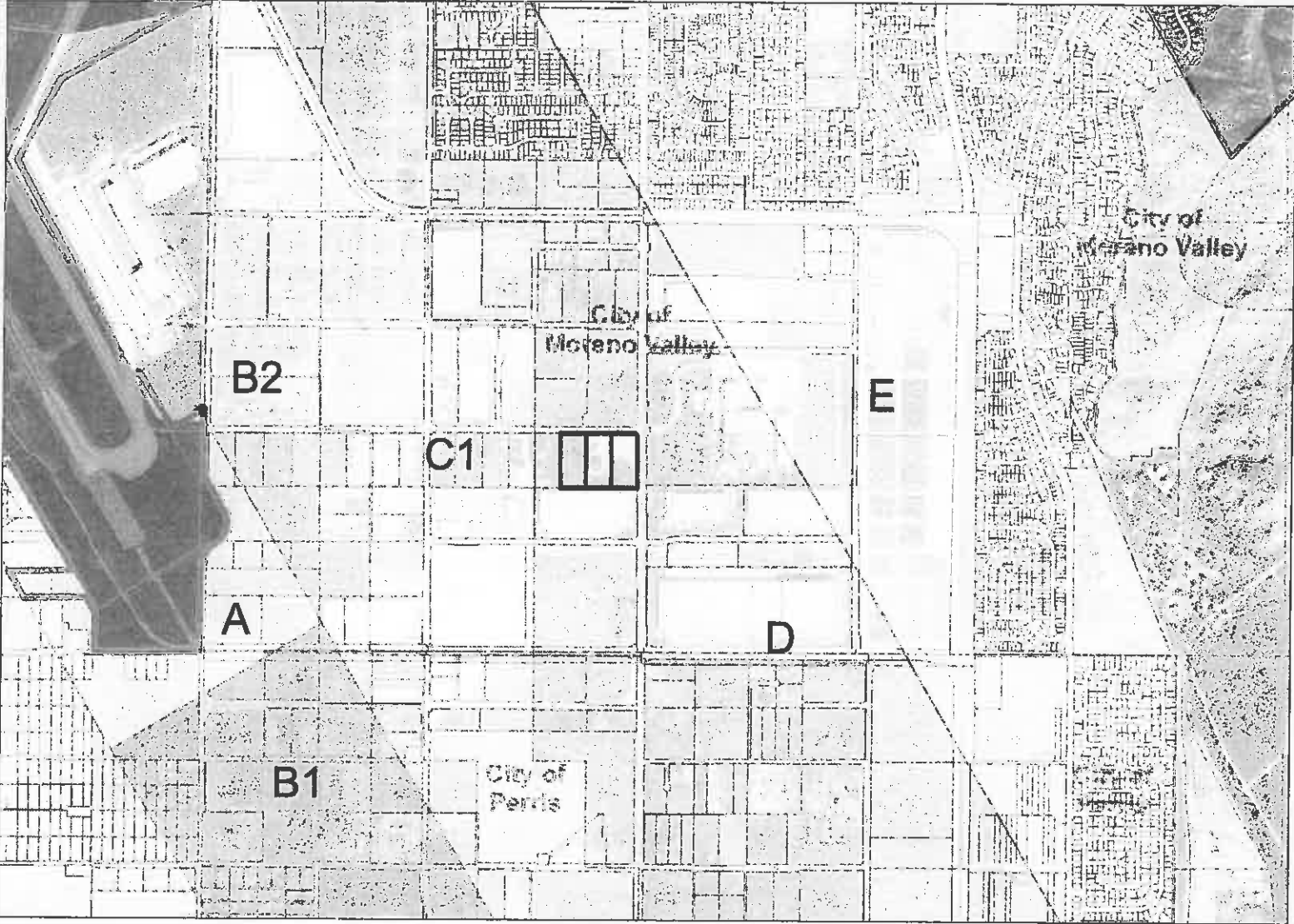
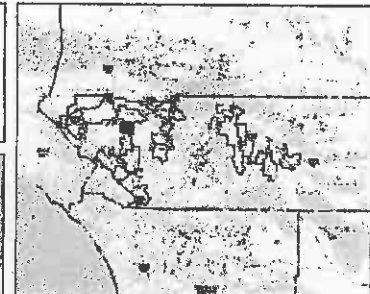
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## Notes

# My Map



**Legend**

Airports  
 AIA  
**Airport Compatibility**

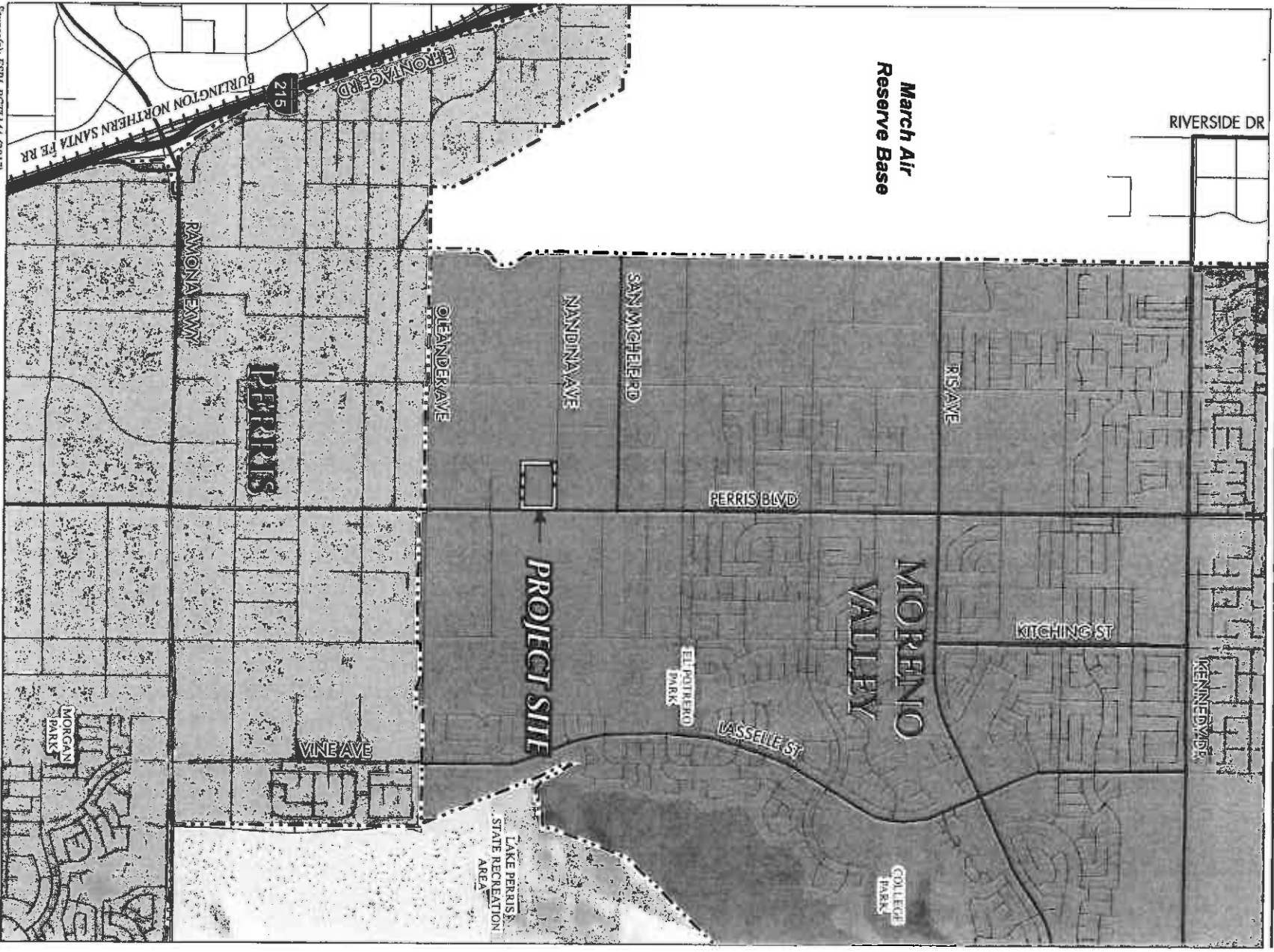
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



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**Notes**

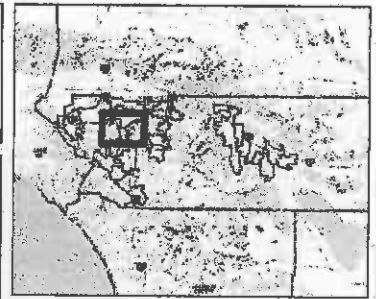
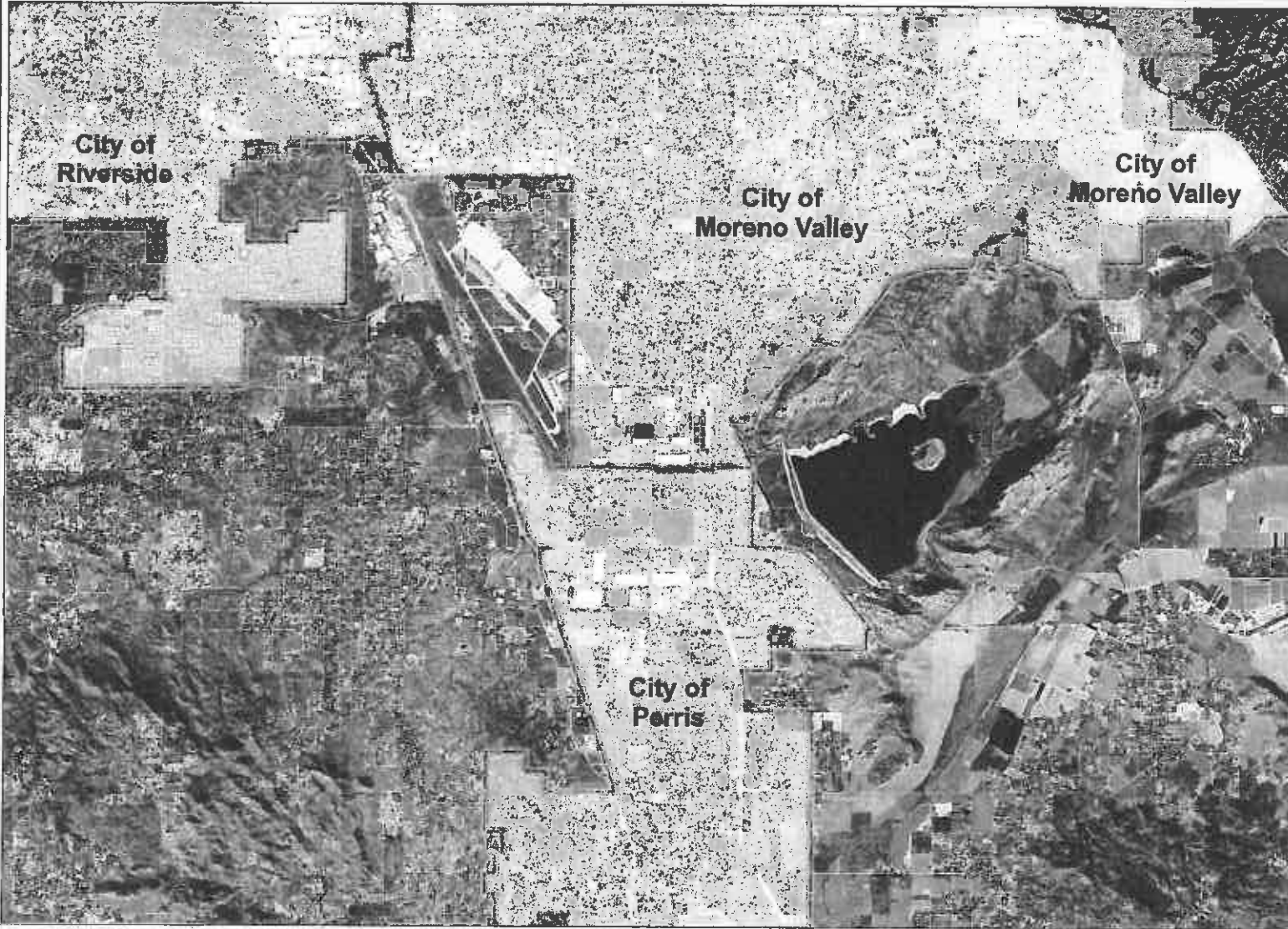
**NANDINA INDUSTRIAL CENTER  
CITY OF MORENO VALLEY**



T&B Planning, Inc.

VICINITY MAP

# My Map



## Legend

- City Boundaries
- Cities**
- adjacent\_highways**
- Interstate
- Interstate 3
- State Highways: 60
- State Highways 3
- US HWY
- OUT
- highways\_large**
- HWY
- INTERCHANGE
- INTERSTATE
- USHWY
- counties
- cities



0 12,129 24,259 Feet



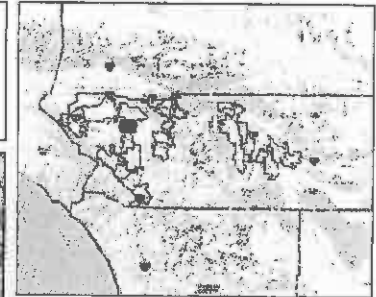
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## Notes

# My Map



## Legend

- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 3,032 6,065 Feet



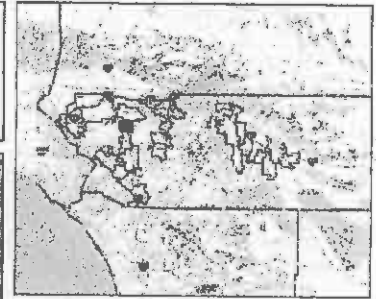
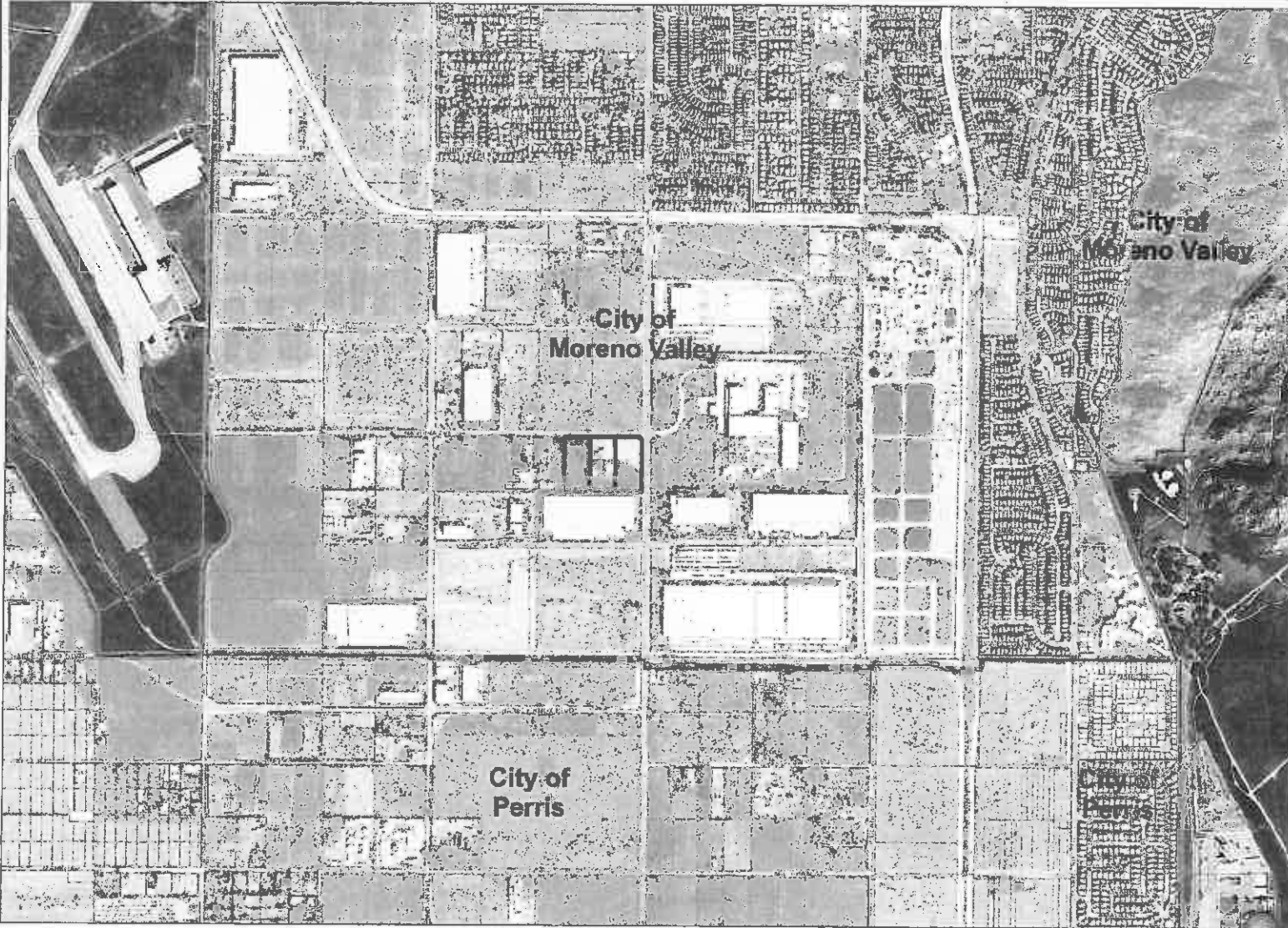
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## Notes

# My Map



## Legend

- City Boundaries
- Cities**
- roads
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads**
- Major Roads
- Arterial
- Collector
- Residential
- counties
- cities
- hydrography
- lines
- waterbodies
- Lakes
- Rivers



0

2,274

4,549 Feet



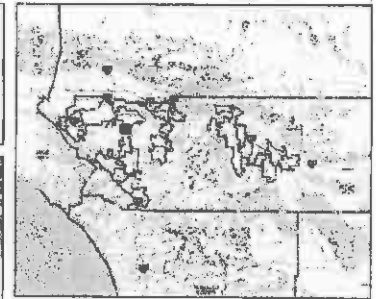
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## Notes

# My Map



## Legend

- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 1,516 3,032 Feet



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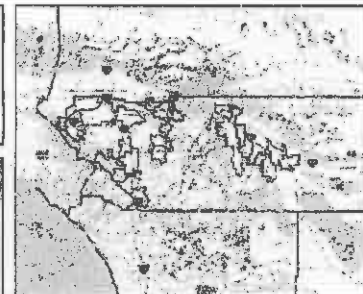
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## Notes



# My Map



## Legend

- City Boundaries
- Cities**
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 379 758 Feet



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## Notes

# **ALUC Review Application - Nandina Industrial Center**

**City of Moreno Valley – ALUC Case No. ZAP-1287MA17**

## **PROJECT DESCRIPTION (REVISED)**

The project site comprises an approximately 14.9-acre, previously disturbed property located immediately south of Nandina Avenue and immediately west of Perris Boulevard in the City of Moreno Valley. The project site is located on the following three Assessor's Parcel Numbers (APNs): 316-210-036, -037, and -038. The Project Applicant proposes to construct an approximately 340,010-square foot (s.f.) warehouse facility with approximately 40 dock doors (positioned on the south side of the facility). The proposed facility would consist of 336,010 s.f. of warehouse floor area and 4,000 s.f. of mezzanine/office space. The facility would also install a pump house, which would support the warehouse building's Early Suppression Fast Response (ESFR) fire protection system, and a guard shack.

Vehicular access to the Project site would be provided by three driveways – two driveways at Nandina Avenue and one driveway at Perris Boulevard. The proposed driveway at Perris Boulevard would be used for truck traffic access only and would be restricted to right turns when exiting the site. The proposed driveways at Nandina Avenue would have no restrictions for passenger vehicle turning movements; however, the eastern driveway would only be used by passenger vehicles while the western driveway could be used for truck traffic and passenger vehicles. All Project driveways would be stop-sign controlled.

The proposed warehouse building would be constructed to a maximum height of approximately 50 feet above finished grade (measuring to the top of the parapets). The building would be constructed with painted concrete tilt-up panels and low-reflective, blue-glazed glass. Articulated building elements, including parapets, wall recesses, mullions and aluminum canopies, are proposed as decorative elements. The exterior color palette for the proposed building is comprised of various neutral colors, including shades of white, gray, and blue. An eight (8)-foot tall, tube steel fence, painted black, would be installed along the majority of the western boundary of the Project site and an eight (8)-foot tall, painted concrete screen wall would be installed along the southern portion of the western boundary of the Project site and along the majority of the southern boundary of the Project site. In addition, the eight (8)-foot tall, painted concrete screen wall at the southern boundary would transition to a five (5)-foot tall, tube steel fence, painted black (abutting the landscaped portion at the southeast corner of the site).

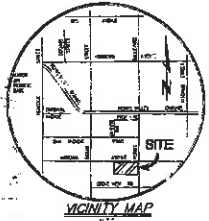
Proposed landscaping would be ornamental in nature. Landscaping would feature drought-tolerant trees, shrubs, and groundcovers. The landscape plan indicates that trees and groundcover are proposed along the Project site's frontages with Nandina Avenue and Perris Boulevard. At building entries and driveways a variety of trees and groundcover would be used to partially shade the structure and parking areas.











NANDINA AVE.

PERRIS BLVD.

1468.93 FF  
1468.26 PAD  
LEVEL FLOOR  
8" SLAB

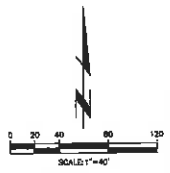
07/2017

**SUBMITTAL BALANCE CALCULATIONS**

PROJECT: DUKE NANDINA AVENUE

JOB# 3583

K.	SITE AREA:	381,434	SF
L.	SUBSIDENCE FACTOR:	0.75%	
M.	SHRINKAGE FACTOR:	1.00%	
N.	SITE STRIPPING FACTOR:	0.00%	
D.	OVEREXCAVATION:	56,075	CY
A.	CALCULATED CUT:	17,229	CY
B.	FOOTING AND UTILITY SPOILS:	6,399	CY
BT.	OFF-SITE SPOILS (STREET AND STORM DRAIN):	1,286	CY
C.	TOTAL CUT: (A+B):	24,914	CY
D.	CALCULATED FILL:	11,008	CY
E.	LIGHT PAVING FILL:	6,399	CY
F.	SUBSIDENCE: (L*F)*C:	2,200	CY
G.	SHRINKAGE: (M*G)*C:	3,841	CY
H.	SITE STRIPPING:	563	CY
I.	OVEREXCAVATION SHRINKAGE:	7,517	CY
J.	TOTAL FILL: (D+H+I+G+H):	23,878	CY
K.	TOTAL (IMPORT) OR EXPORT:	0	CY
	SITE ALIGNMENT:	8.00	



PREPARED FOR:

DUKE REALTY  
300 VICTORIA CIRCLE, SUITE 100  
IRVINE, CA 92614  
PHONE: (949) 757-7200  
FAX: (949) 757-7000



CITY OF MORENO VALLEY  
PUBLIC WORKS DEPARTMENT

**CONCEPTUAL GRADING PLAN**

DUKE REALTY  
NANDINA AVENUE

Designed by	_____	Approved by	_____
Date	_____	Scale	_____
Checked by	_____	Title	_____
Drawn by	_____	Table	_____
Scale	_____	Sheet	_____
Drawn by	_____	Scale	_____
Date	_____	Sheet	_____

1 of 4 SHEET

PAGE BREAK





# AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY



November 2, 2017

**CHAIR**  
**Rod Ballance**  
Riverside

**VICE CHAIRMAN**  
**Steve Manos**  
Lake Elsinore

**COMMISSIONERS**

**Arthur Butler**  
Riverside

**John Lyon**  
Riverside

**Glen Holmes**  
Hemet

**Russell Betts**  
Desert Hot Springs

**Steven Stewart**  
Palm Springs

**STAFF**

**Director**  
**Simon A. Housman**

**John Guerin**  
**Paul Rull**  
**Barbara Santos**

County Administrative Center  
4080 Lemon St., 14th Floor  
Riverside, CA 92501  
(951) 955-5132

[www.rcaluc.org](http://www.rcaluc.org)

Ms. Candice Assadzadeh, Project Planner  
City of Riverside Community Development Department/Planning Division  
3900 Main Street, Third Floor  
Riverside, CA 92522

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW --  
DIRECTOR'S DETERMINATION**

File No.: ZAP1290MA17  
Related File No.: P17-0078 (Conditional Use Permit)  
APN: 211-060-018

Dear Ms. Assadzadeh:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed City of Riverside Case Nos. P17-0078 (Conditional Use Permit), a proposal to establish a church facility within an existing 5,400 square foot commercial building on a 6.04 acre parcel located at 3330 Durahart Street (on the east side of Durahart Street, southerly of 3<sup>rd</sup> Street and westerly of Chicago Avenue).

The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, non-residential intensity is not restricted.

The elevation of the site is more than 500 feet lower than the elevation of March's Runway 14-32 at its northerly terminus (approximately 1,535 feet above mean sea level). The site is located more than 20,000 feet from the runways at Riverside Municipal Airport. The site is located 16,725 feet from the northeasterly terminus of the runway at Flabob Airport, but the notice radius for that airport is 10,000 feet, since the runway length does not exceed 3,200 feet. Therefore, Federal Aviation Administration Obstruction Evaluation Service review for height/elevation reasons was not required.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, provided that the City of Riverside applies the following recommended conditions:

**CONDITIONS:**

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.

## AIRPORT LAND USE COMMISSION

2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris centers, fly ash disposal, and incinerators.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached notice shall be provided to all potential purchasers of the property and to tenants of the building(s) thereon.
4. No detention basins are depicted on the site plan. Any new aboveground detention or water quality basins on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention/water quality basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.

If you have any questions, please contact Paul Rull, ALUC Urban Regional Planner IV, at (951) 955-6893 or John Guerin, ALUC Principal Planner, at (951) 955-0982.

Sincerely,

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



---

Simon A. Housman, ALUC Director

Attachments: Notice of Airport in Vicinity

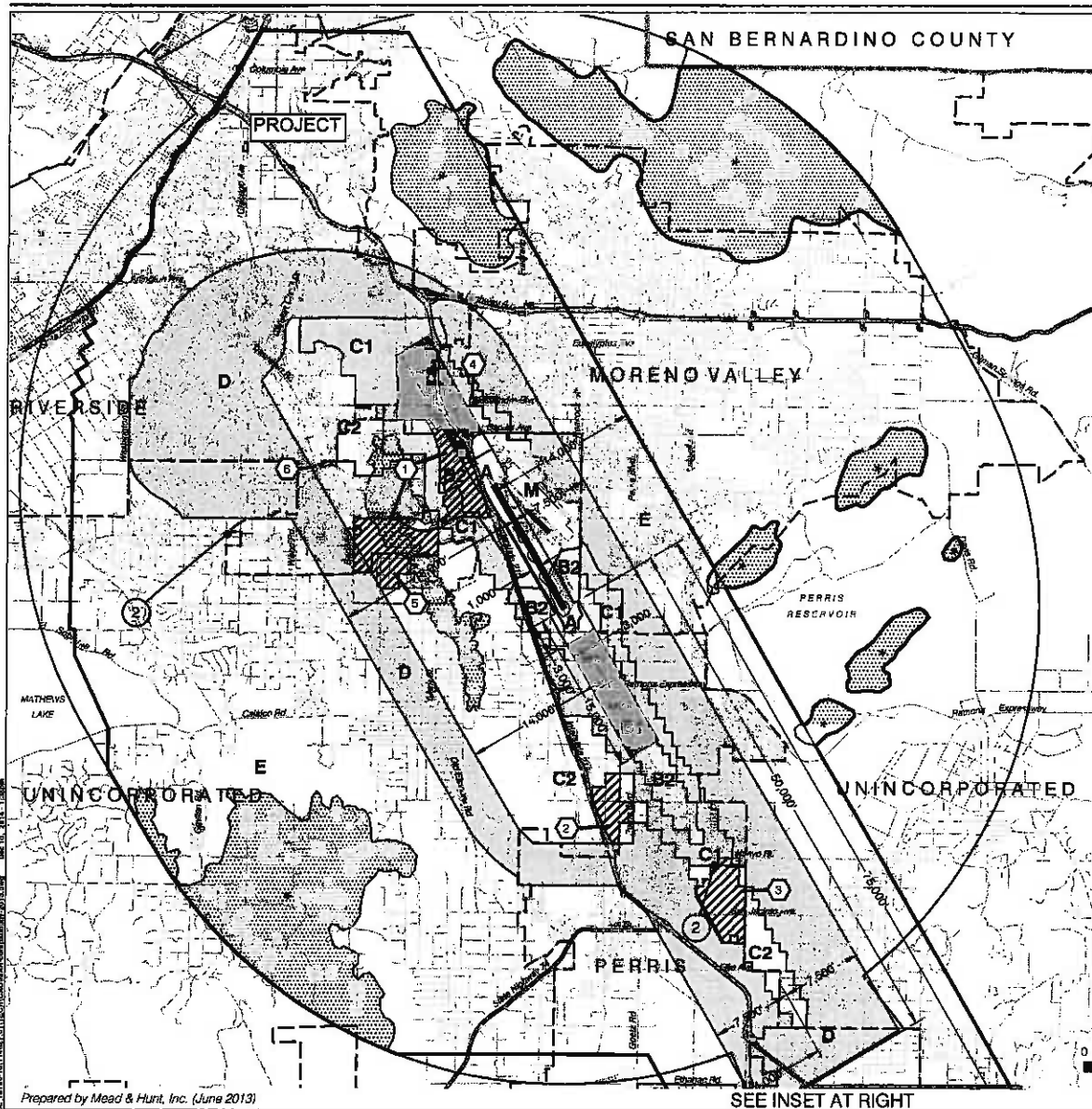
## **AIRPORT LAND USE COMMISSION**

cc: Matthew Thronson (applicant)  
Martin Commercial Group (Riverside office)  
New Beginnings Christian Ministries (fee-payer)  
Martin Investment Co., c/o Industrial Property Management (listed owner– GIS/Assessor)  
Gary Gosliga, Airport Manager, March Inland Port Airport Authority  
Denise Hauser or Daniel Rockholt, March Air Reserve Base  
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1290MA17\ZAP1290MA17.LTR.doc

# **NOTICE OF AIRPORT IN VICINITY**

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



**LEGEND**

**Compatibility Zones**

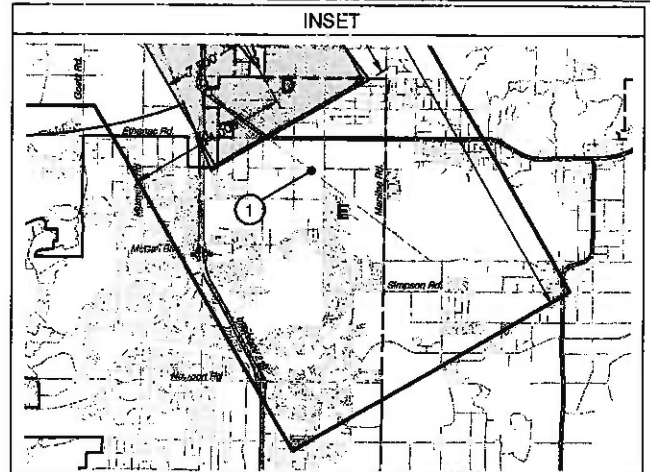
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

**Boundary Lines**

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- ▨ Site-Specific Exceptions (existing local agency commitments to development projects)

- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



**Riverside County  
Airport Land Use Commission  
March Air Reserve Base / Inland Port Airport  
Land Use Compatibility Plan  
(Adopted November 13, 2014)**

Map MA-1

**Compatibility Map**  
March Air Reserve Base / Inland Port Airport

Note:  
All dimensions are measured from  
runway ends and centerlines.



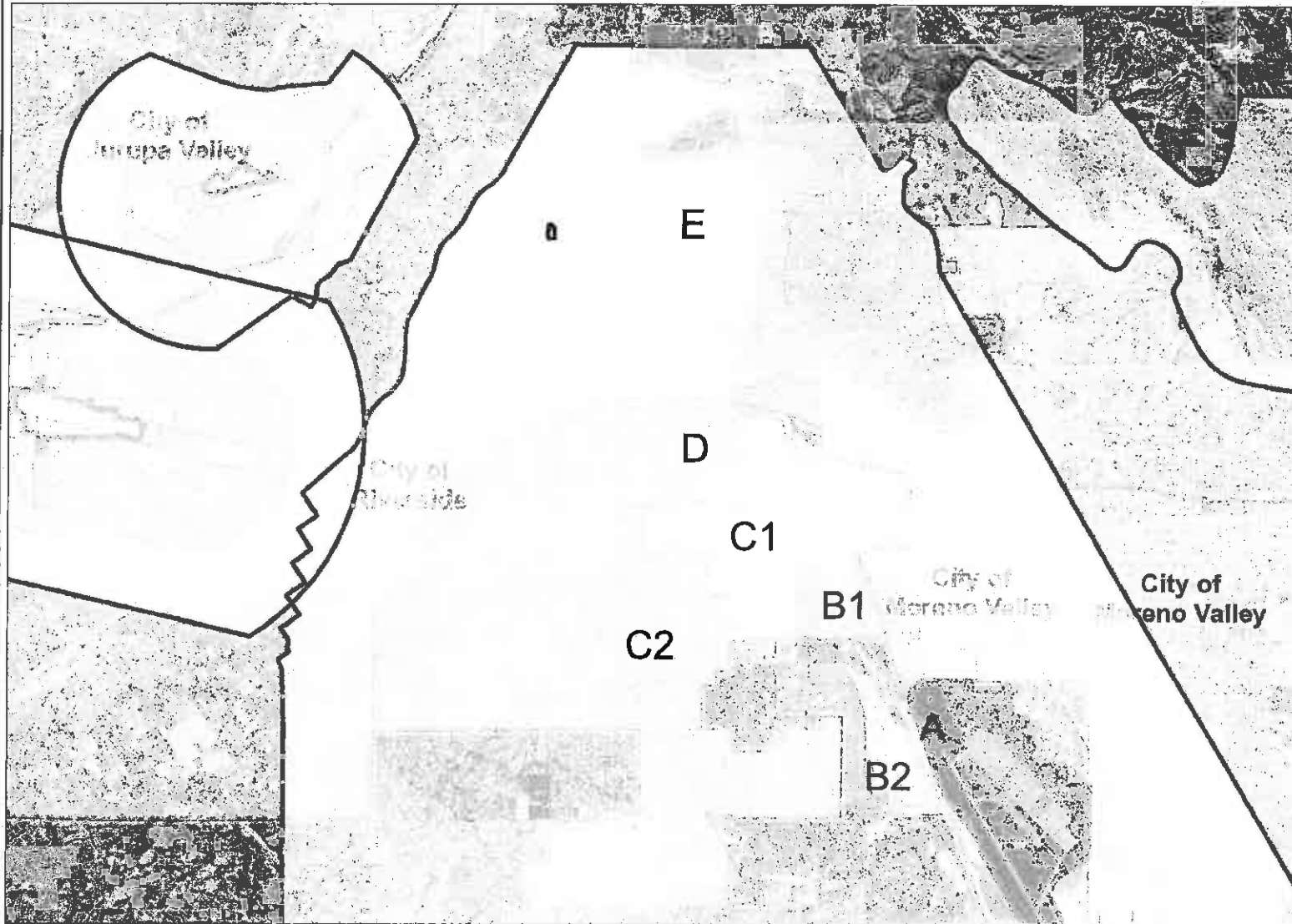
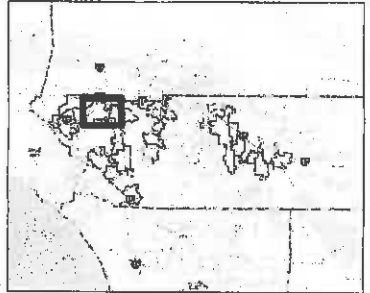
Base map source: County of Riverside 2013

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION - 2015

Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

# My Map



## Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



0 10,991 21,983 Feet



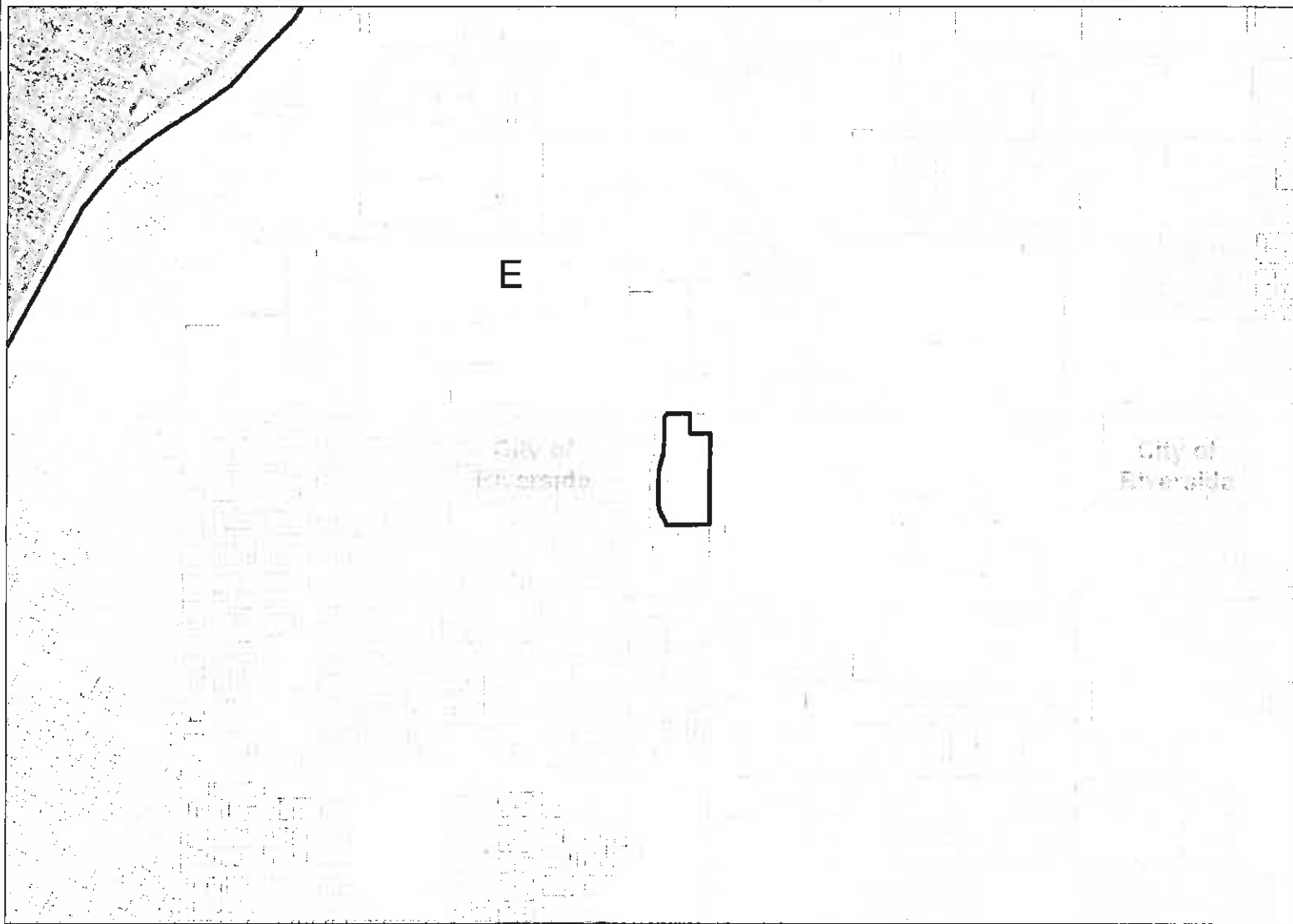
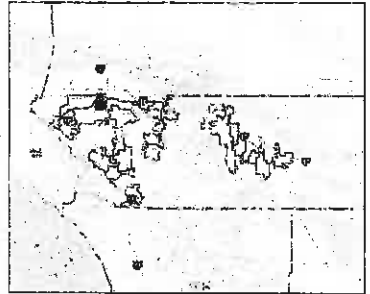
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## Notes

# My Map



### Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
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- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



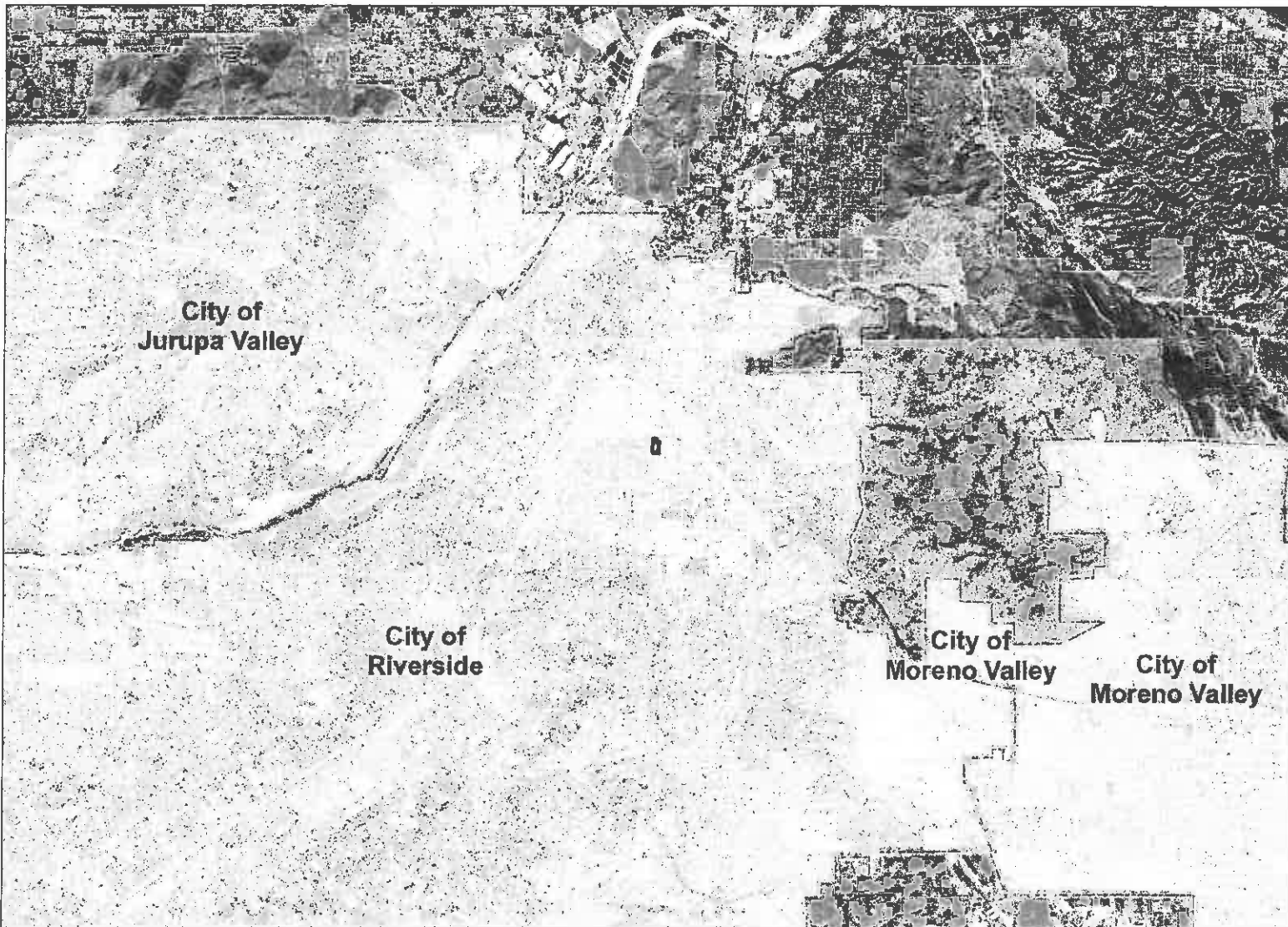
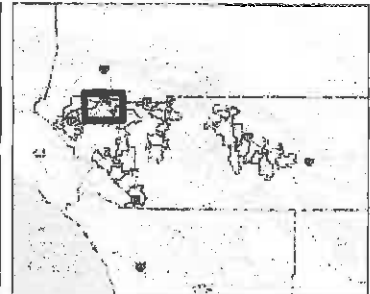
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### Notes

# My Map



### Legend

- City Boundaries
- Cities**
- adjacent\_highways**
  - Interstate
  - Interstate 3
  - State Highways; 60
  - State Highways 3
  - US HWY
  - OUT
- highways\_large**
  - HWY
  - INTERCHANGE
  - INTERSTATE
  - USHWY
- counties
- cities



0 10,991 21,983 Feet



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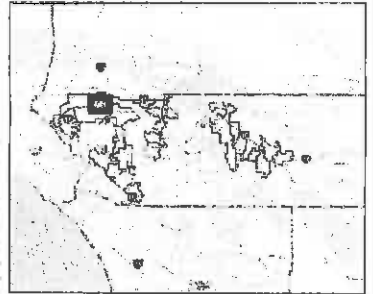
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### Notes



# My Map



## Legend

- City Boundaries
- Cities
- highways\_large
  - HWY
  - INTERCHANGE
  - INTERSTATE
  - USHWY
- majorroads
- counties
- cities
- hydrographylines
- waterbodies
  - Lakes
  - Rivers



0 5,496 10,991 Feet



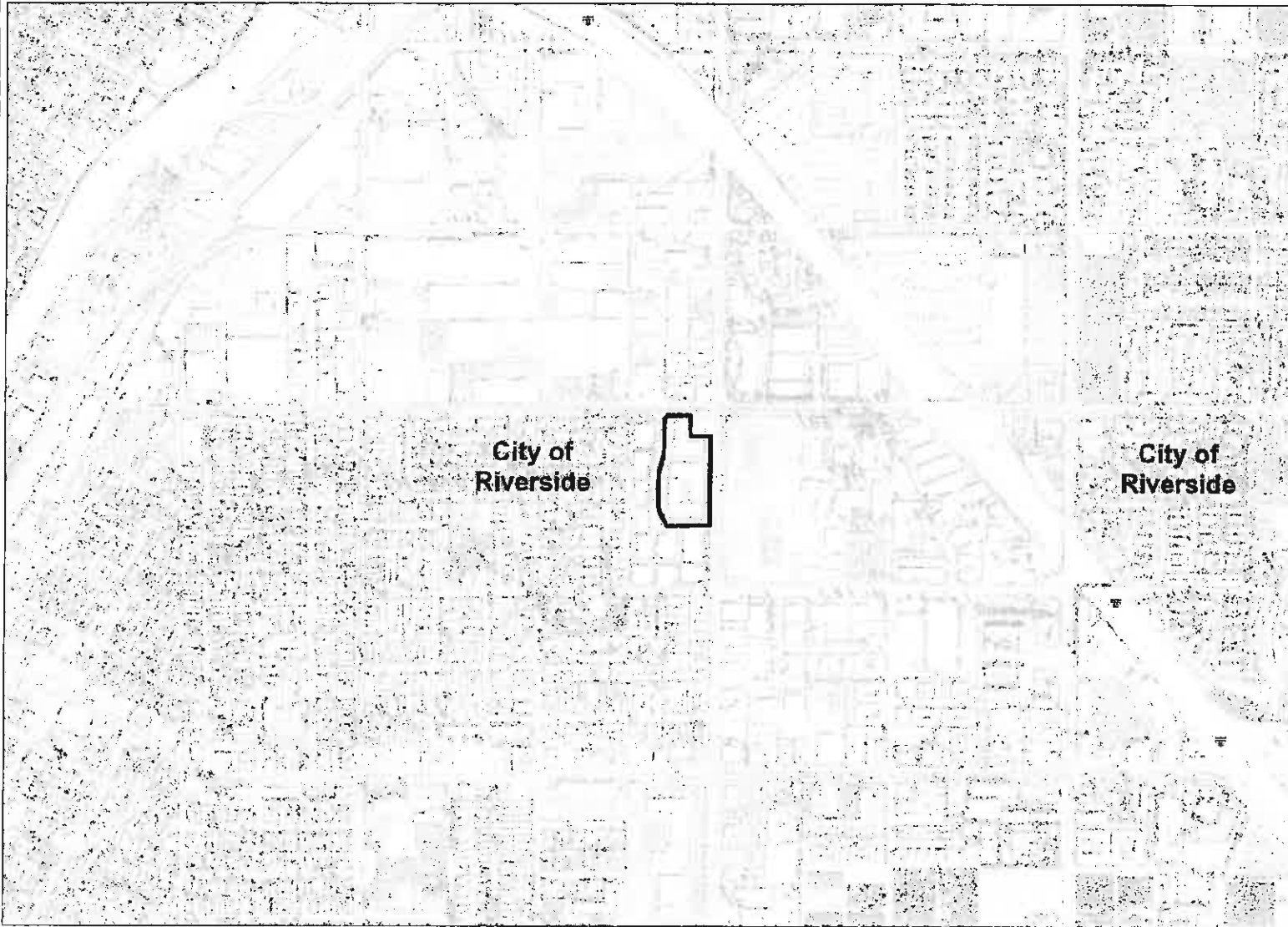
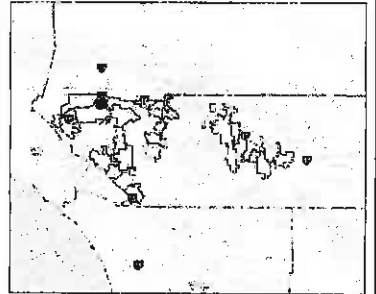
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## Notes

# My Map



## Legend

- City Boundaries
- Cities
- roadsanno
- highways
  - HWY
  - INTERCHANGE
  - INTERSTATE
  - OFFRAMP
  - ONRAMP
  - USHWY
- counties
- cities
- hydrographylines
- waterbodies
  - Lakes
  - Rivers



0 1,374 2,748 Feet



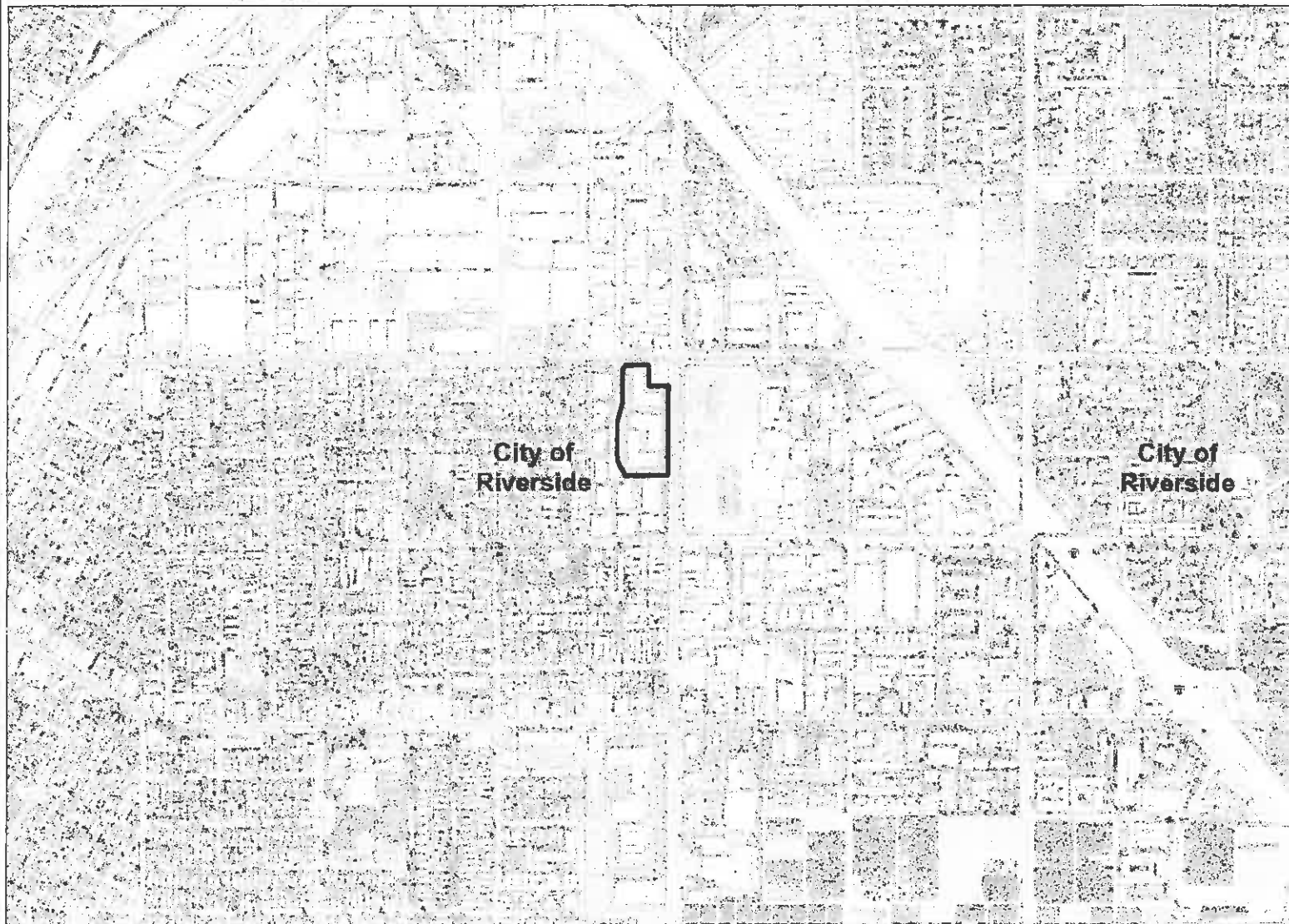
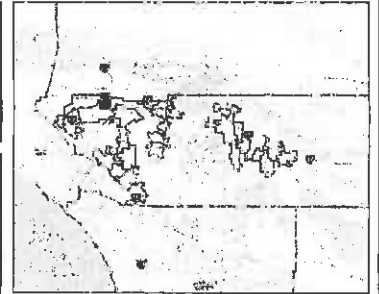
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## Notes

# My Map



## Legend

- City Boundaries
- Cities
- roadsanno
- highways
  - HWY
  - INTERCHANGE
  - INTERSTATE
  - OFFRAMP
  - ONRAMP
  - USHWY
- counties
- cities
- hydrographylines
- waterbodies
  - Lakes
  - Rivers



0 1,374 2,748 Feet



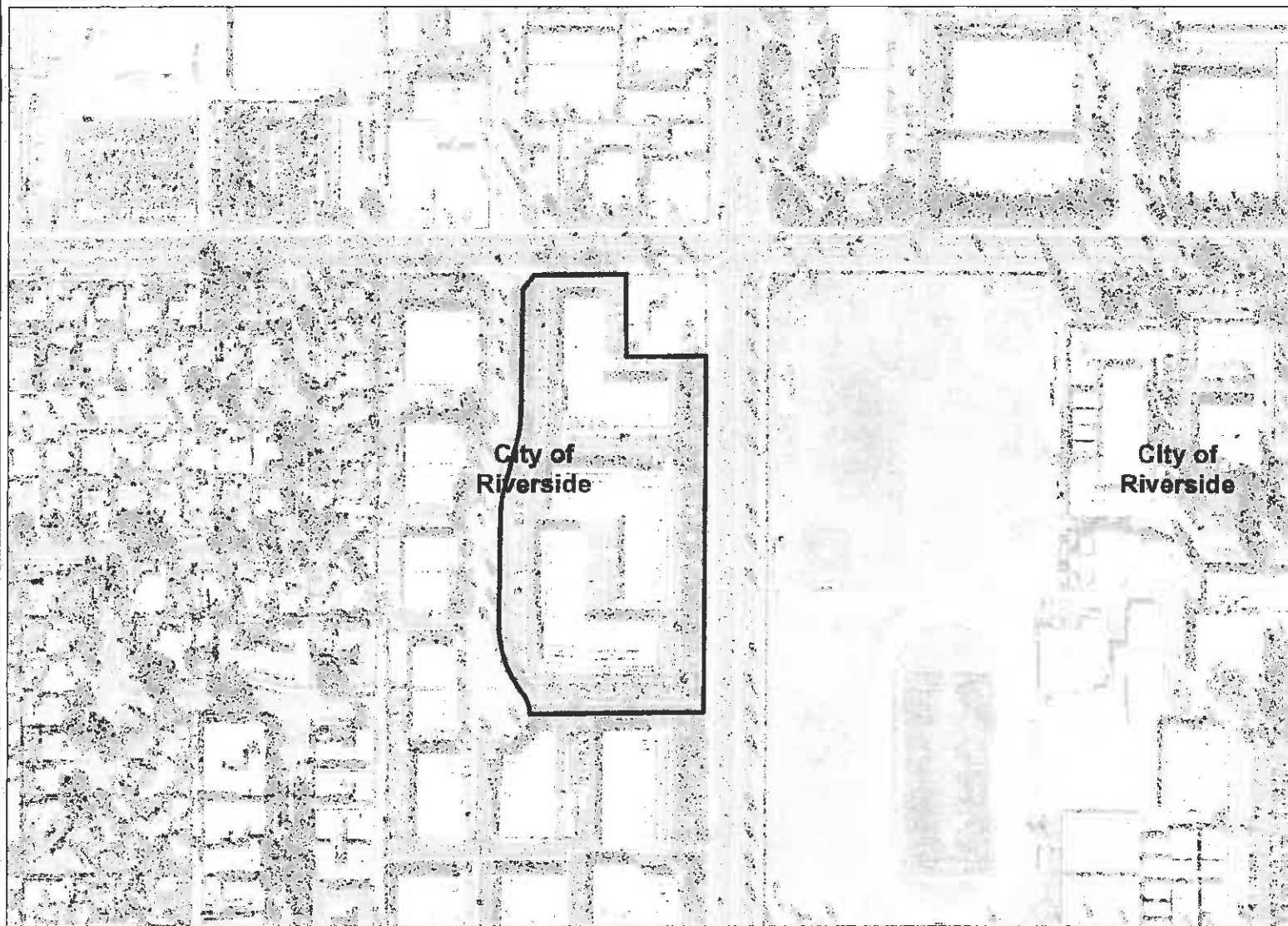
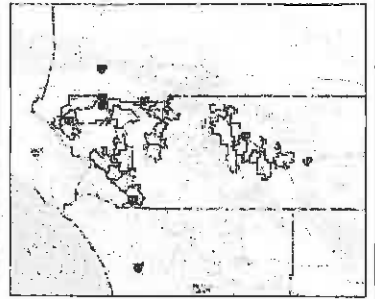
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## Notes

# My Map



## Legend

-  City Boundaries
- Cities
- roadsanno
- highways
  -  HWY
  -  INTERCHANGE
  -  INTERSTATE
  -  OFFRAMP
  -  ONRAMP
  -  USHWY
-  counties
-  cities
- hydrographylines
- waterbodies
  -  Lakes
  -  Rivers



0 343 687 Feet



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## Notes



**Tenant Improvement/  
Interior Alteration**

3330 Outahart Avenue  
Riverside, CA 92507

**Client**  
Matthew Thronson  
New Evangelical Christian Church  
2380 Outahart Avenue  
Riverside, CA 92507  
661-937-0214  
mthronson@yahoo.com

**Architect**  
Gregory J. Korn, AIA  
Greg Korn Architects, P.C.  
2371 North Coker Dr. Ste 202  
Thousand Oaks, CA 91321  
805-499-9849  
gja@grkpa.com

General Contractor:

City Agencies / Plan Examiner  
Catholic Association, Account Planner  
City of Riverside  
2902 Main Street  
Riverside, CA 92502  
951-925-3371

No. Description Date

1 SD For Owner Review 08/26/2017

Project number 2017002

Date 06.25.2017

Drawn by GJK

Checked by GK

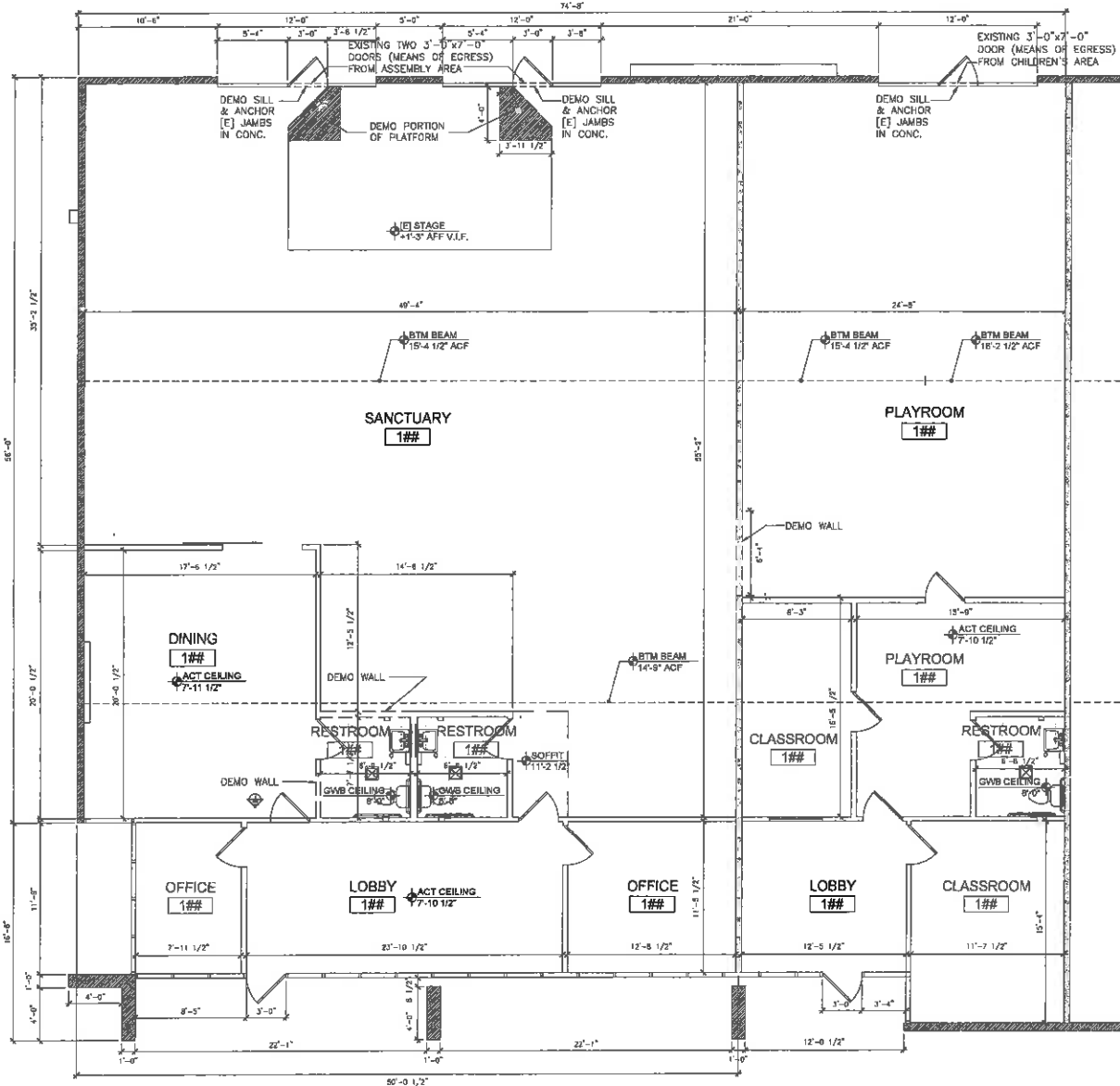
Drawing Name

**EXISTING FLOOR PLAN**

Drawing No.

**A-100.00**

Scale 1/4" = 1'-0" Page # 3 of 4



**EXISTING PLAN KEY**

- [E] CONCRETE BEARING WALL
- [E] LOAD BEARING WALL, 1 HR RATED
- [E] NON LOAD BEARING METAL STUD WALL, NON RATED
- DEMOLISH [E] WALL

NOTE: SEE COVER SHEET FOR GENERAL SYMBOLS AND ABBREVIATIONS KEY



**Tenant Improvement/  
Interior Alteration**

3300 Carroll Avenue  
Burbank, CA 91507

**Client:**  
Santitas, Inc.  
10000 Hollywood Blvd., Suite 1000  
Burbank, CA 91507  
Tel: 818.333.1111  
www.santitas.com

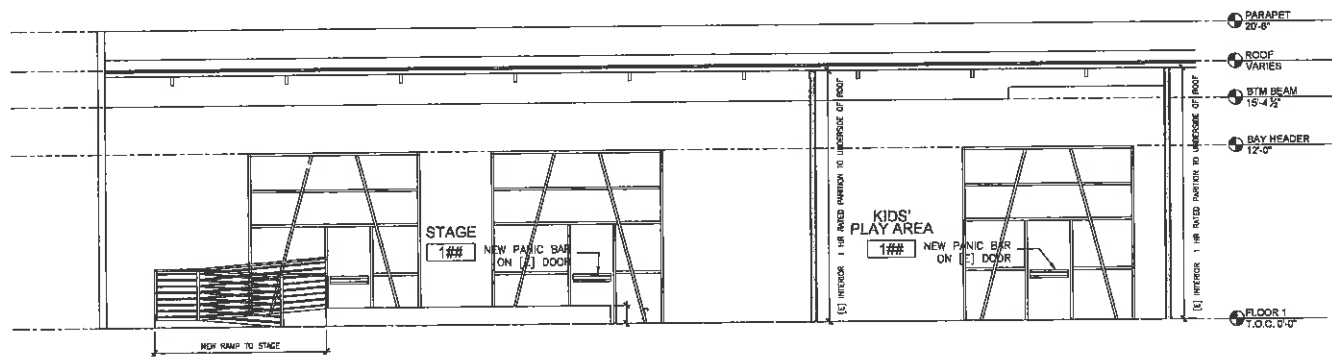
**Architect:**  
Jones Vivian Architects, Inc.  
10000 Hollywood Blvd., Suite 1000  
Burbank, CA 91507  
Tel: 818.333.1111  
www.jv.com

General Contractor:

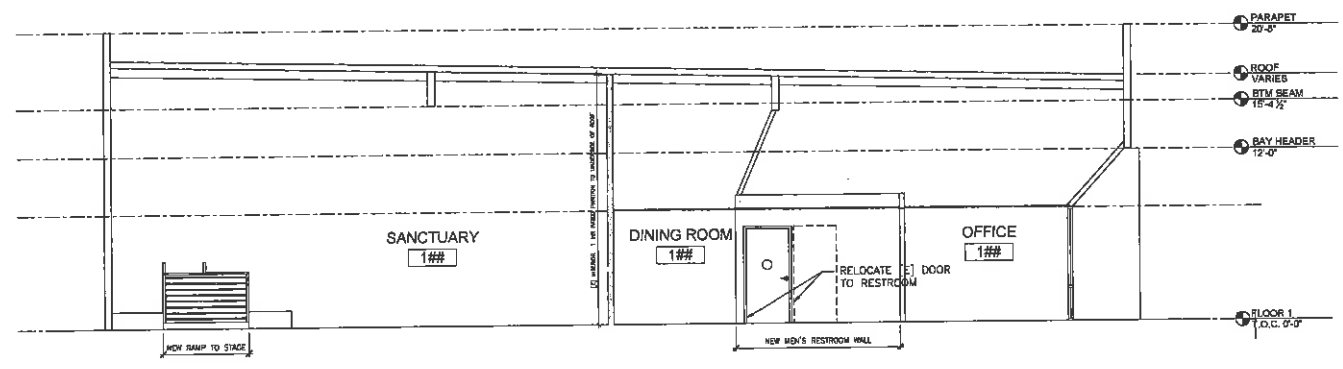
**City Approvals / Plan Number:**  
City of Burbank, Planning Dept.  
10000 Hollywood Blvd., Suite 1000  
Burbank, CA 91507  
Tel: 818.333.1111

No.	Description	Date
1	100% Design Review (20110211)	

Project Number	20110211
Date	08.21.2011
Drawn by	GA
Checked by	GA
Design Name	<b>PROPOSED INTERIOR ELEVNS</b>
Drawing No.	<b>A-401.00</b>
Scale	AS NOTED
Sheet	4 of 4



**Proposed Interior Elevation - Sanctuary and Kids' Play Area - View East**  
Scale 1/4" = 1'-0"



**Proposed Interior Elevation - Sanctuary and Dining Area - View South**  
Scale 1/4" = 1'-0"



PAGE BREAK





# AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

November 9, 2017

**CHAIR**

Rod Ballance  
Riverside

Mr. Gabriel Diaz, Associate Planner

**VICE CHAIRMAN**

Steve Manos  
Lake Elsinore

City of Moreno Valley Community Development Department

14177 Frederick Street  
Moreno Valley CA 92553

**COMMISSIONERS**

Arthur Butler  
Riverside

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –  
DIRECTOR'S DETERMINATION**

John Lyon  
Riverside

File No.: ZAP1292MA17  
Related File No.: PEN17-0114 (Site Plan Review)

Glen Holmes  
Hemet

APNs: 316-190-012 and -049

Russell Betts  
Desert Hot Springs

Dear Mr. Diaz:

Steven Stewart  
Palm Springs

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed City of Moreno Valley Case No. PEN17-0114 (Site Plan Review), a proposal to establish a truck storage facility with 93 truck/trailer stalls (involving conversion of an existing 3,034 square foot structure constructed as a four-bedroom home into an office) on 4.89 acres with an address of 24811 Rivard Road, on the southerly side of Rivard Road, westerly of Perris Boulevard.

**STAFF**

**Director**

Simon A. Housman

John Guerin  
Paul Rull

Barbara Santos

The site is located within Airport Compatibility Zone D of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone D of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, nonresidential intensity is not restricted.

County Administrative Center  
4080 Lemon St., 14th Floor.  
Riverside, CA 92501  
(951) 955-5132

[www.rcaluc.org](http://www.rcaluc.org)

The elevation of Runway 14-32 at March Air Reserve Base/Inland Port Airport at its southerly terminus is approximately 1,488 feet above mean sea level (1488 AMSL). At a distance of approximately 5,962 feet from the runway to the project property line, Federal Aviation Administration Obstruction Evaluation Services (FAA OES) review would be required for any structures with top of roof exceeding 1,557 feet AMSL. The project's site elevation is 1,474 feet AMSL. The tallest new structure would be a 20 foot parking lot light standard, resulting in a top point elevation of 1,494 feet AMSL. Therefore, review by the FAA OES was not required.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, provided that the City of Moreno Valley applies the following recommended conditions:

**CONDITIONS:**

## AIRPORT LAND USE COMMISSION

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site.
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached notice shall be provided to all potential purchasers of the property and to tenants of the building(s) thereon, and shall be recorded as a deed notice.
4. The proposed detention basins on the site (including water quality management basins) shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.
5. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment of irrigation controllers, access gates, etc.

If you have any questions, please contact Paul Rull, ALUC Urban Regional Planner IV, at (951) 955-6893 or John Guerin, ALUC Principal Planner, at (951) 955-0982.

## AIRPORT LAND USE COMMISSION

Sincerely,  
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



---

Simon A. Housman, ALUC Director

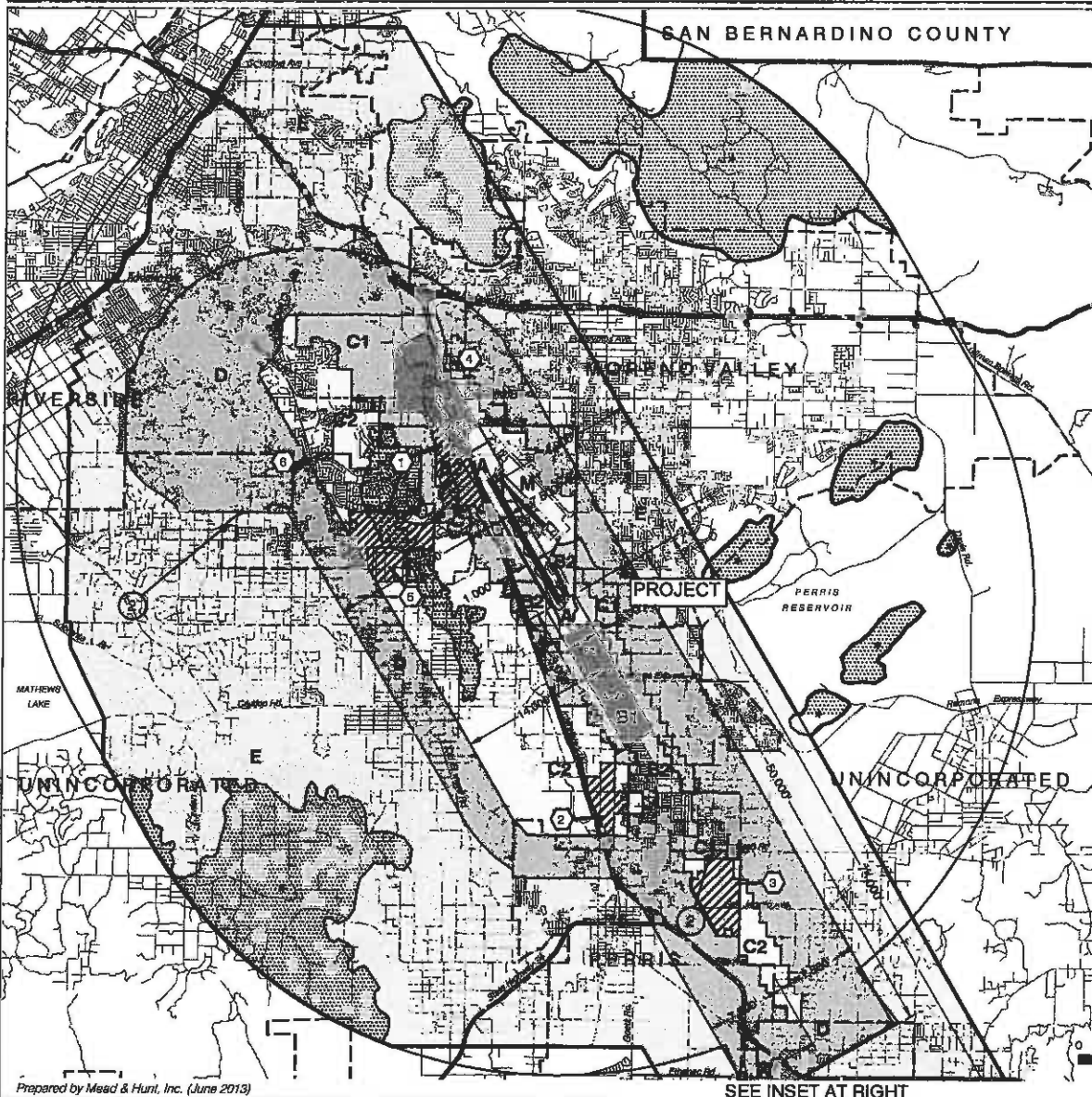
Attachments: Notice of Airport in Vicinity

cc: Balwinder Kang, Cargo Solution Express, Inc. (applicant/property owner)  
Inland Engineers & Associates, Shoab Siddiqui (representative)  
Gary Gosliga, Airport Manager, March Inland Port Airport Authority  
Denise Hauser or Daniel Rockholt, March Air Reserve Base  
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1292MA17\ZAP1292MA17.LTR.doc

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



**LEGEND**

**Compatibility Zones**

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

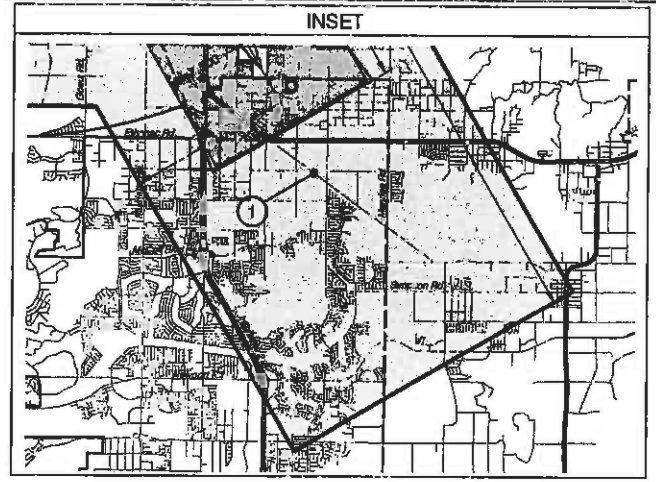
**Boundary Lines**

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- ▨ Site-Specific Exceptions (existing local agency commitments to development projects)

① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.

② Point at which departing aircraft typically reach 3,000 feet above runway end.

① March JPA: March Business Center/Meridian  
 ② Perris: Harvest Landing  
 ③ Perris: Park West  
 ④ Moreno Valley: Affordable Housing  
 ⑤ March JPA: Ban Clark Training Center  
 ⑥ Riverside: Ridge Crest Subdivision



Riverside County  
 Airport Land Use Commission  
 March Air Reserve Base / Inland Port Airport  
 Land Use Compatibility Plan  
 (Adopted November 13, 2014)

Note:  
 All dimensions are measured from  
 runway ends and centerlines.



Base map source: County of Riverside 2013

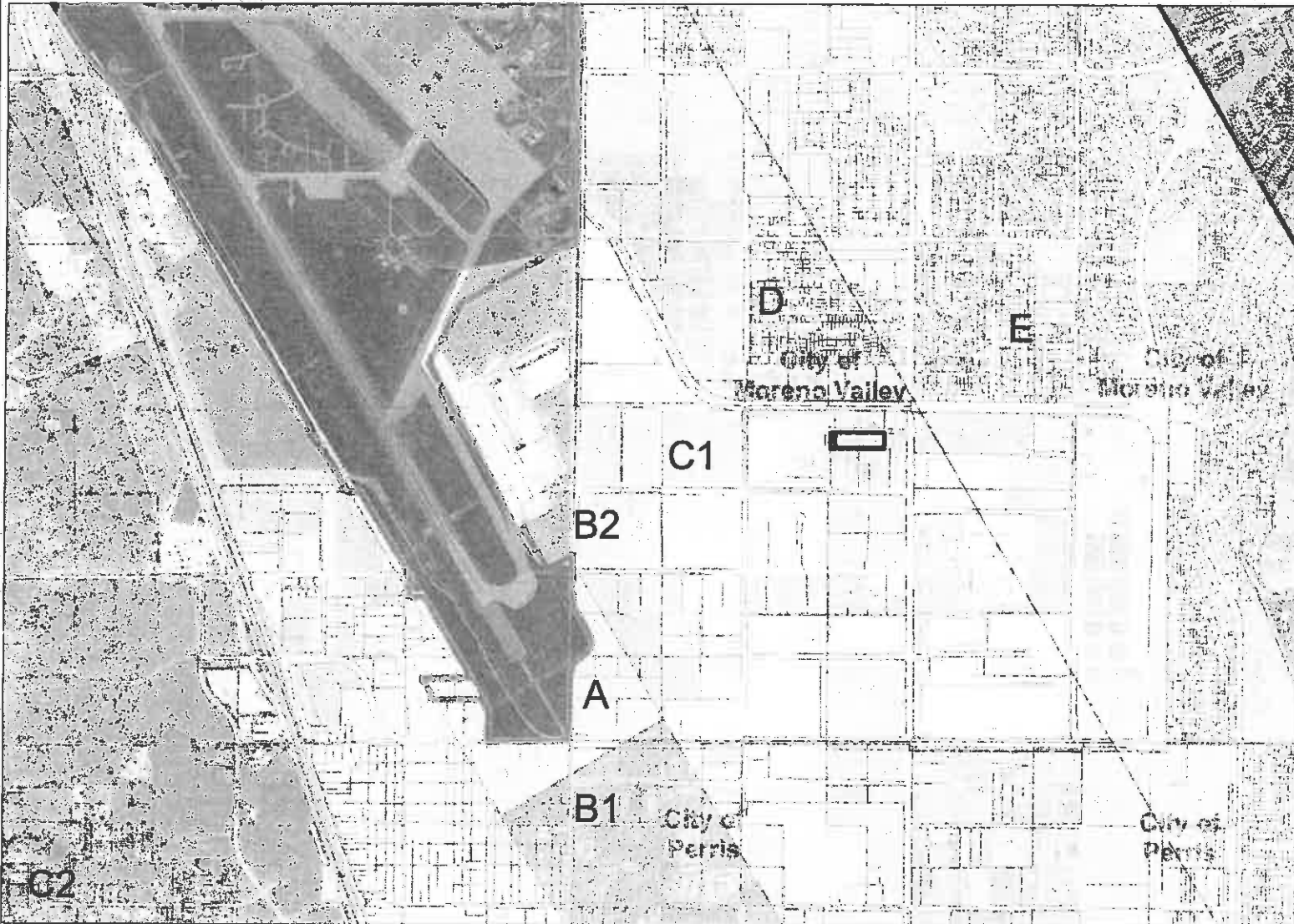
Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map MA-1

**Compatibility Map**  
 March Air Reserve Base / Inland Port Airport

# My Map

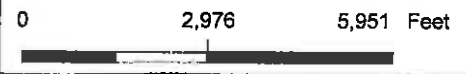


**Legend**

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



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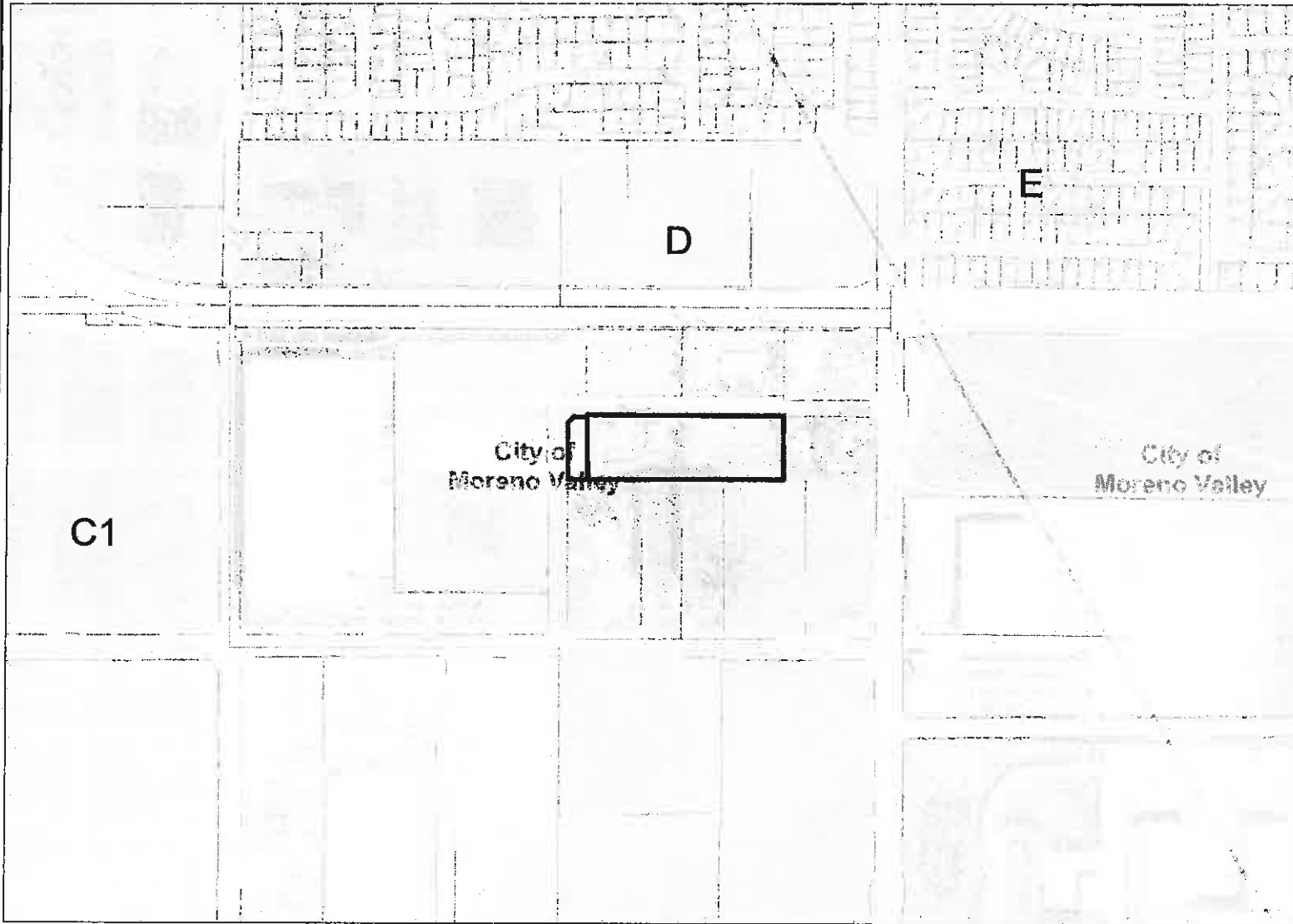
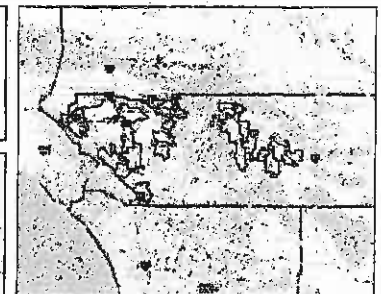


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**Notes**

# My Map



## Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ1
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC8
- C2-HIGHT



0 744 1,488 Feet



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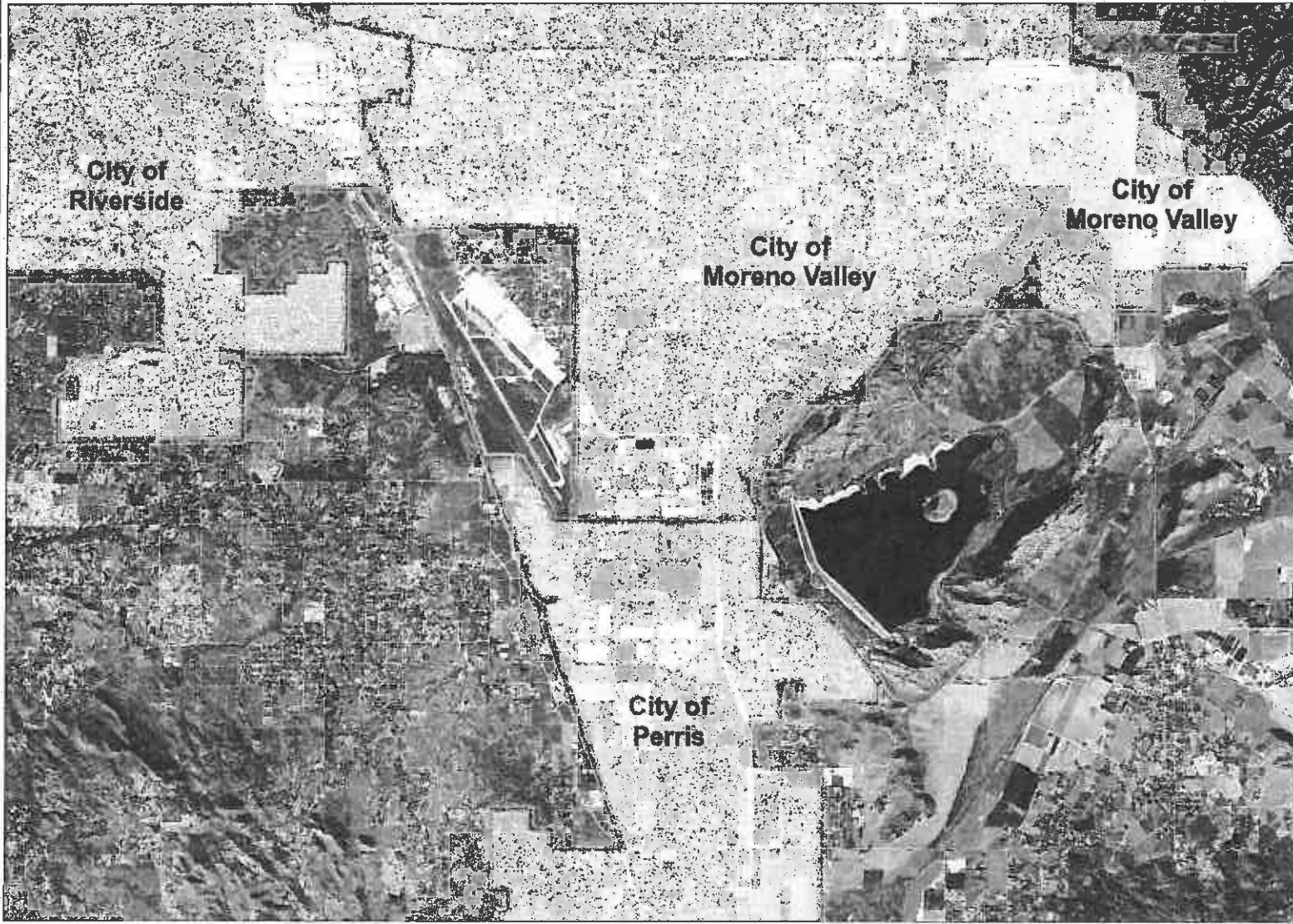
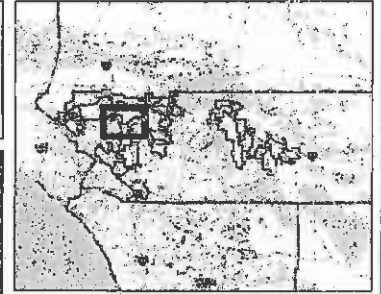
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## Notes



# My Map



## Legend

-  City Boundaries
- Cities**
- adjacent\_highways**
  -  Interstate
  -  Interstate 3
  -  State Highways, 60
  -  State Highways 3
  -  US HWY
  -  OUT
- highways\_large**
  -  HWY
  -  INTERCHANGE
  -  INTERSTATE
  -  USHWY
-  counties
-  cities



0 11,902 23,804 Feet



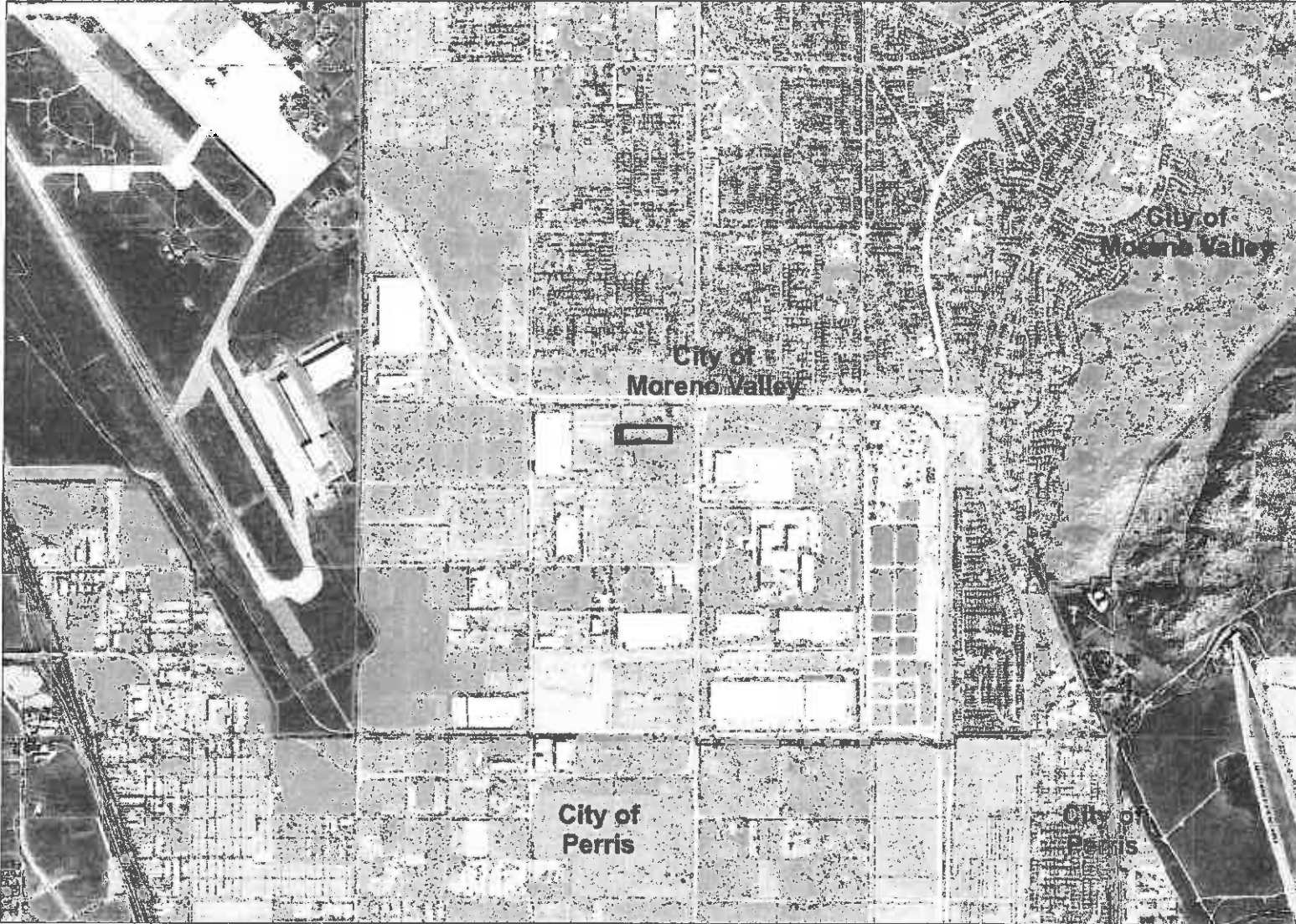
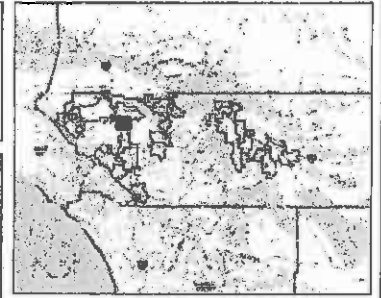
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## Notes

# My Map



- Legend**
- City Boundaries
  - Cities
  - roads
  - highways
  - HWY
  - INTERCHANGE
  - INTERSTATE
  - OFFRAMP
  - ONRAMP
  - USHWY
  - roads
  - Major Roads
  - Arterial
  - Collector
  - Residential
  - counties
  - cities
  - hydrography
  - lines
  - waterbodies
  - Lakes
  - Rivers



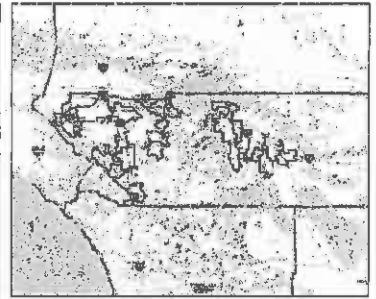
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## Notes

# My Map



## Legend

- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



0 744 1,488 Feet



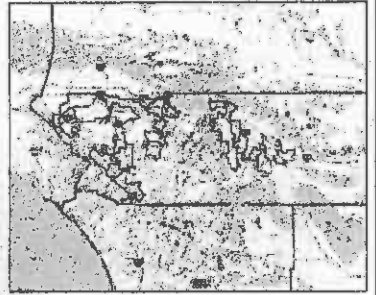
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## Notes

# My Map



## Legend

-  City Boundaries
- Cities
- roadsanno
- highways
  -  HWY
  -  INTERCHANGE
  -  INTERSTATE
  -  OFFRAMP
  -  ONRAMP
  -  USHWY
-  counties
-  cities
- hydrographylines
- waterbodies
  -  Lakes
  -  Rivers



0

372

744 Feet

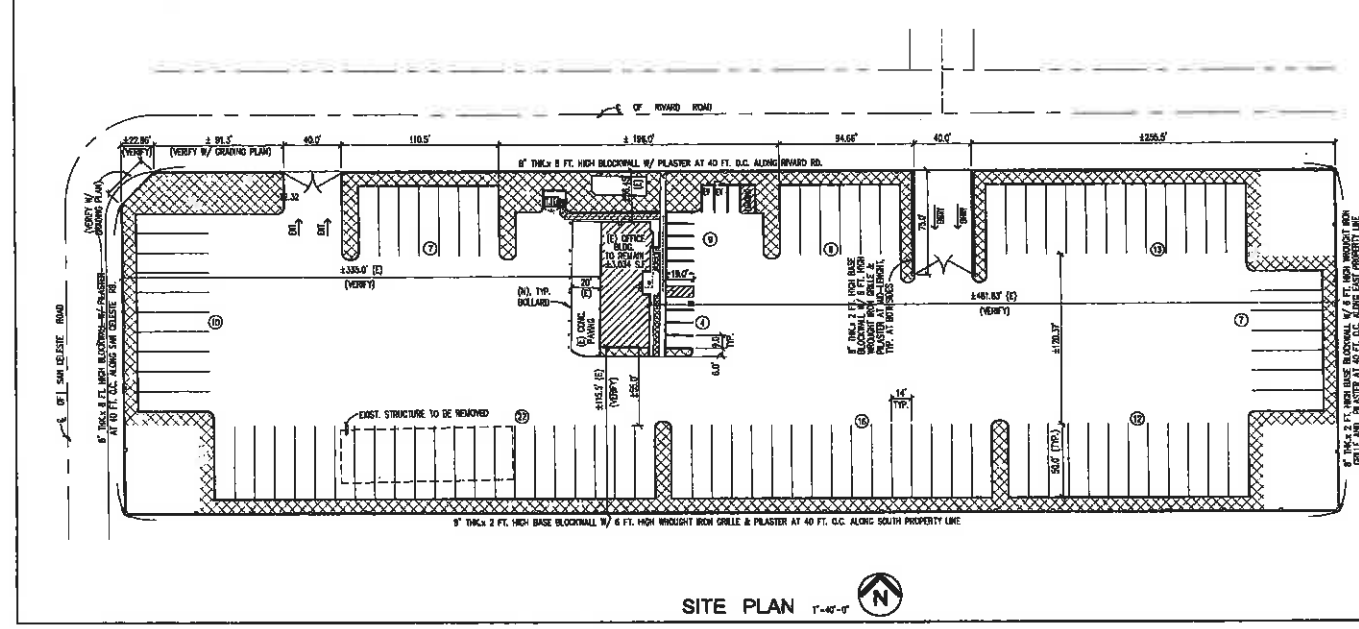
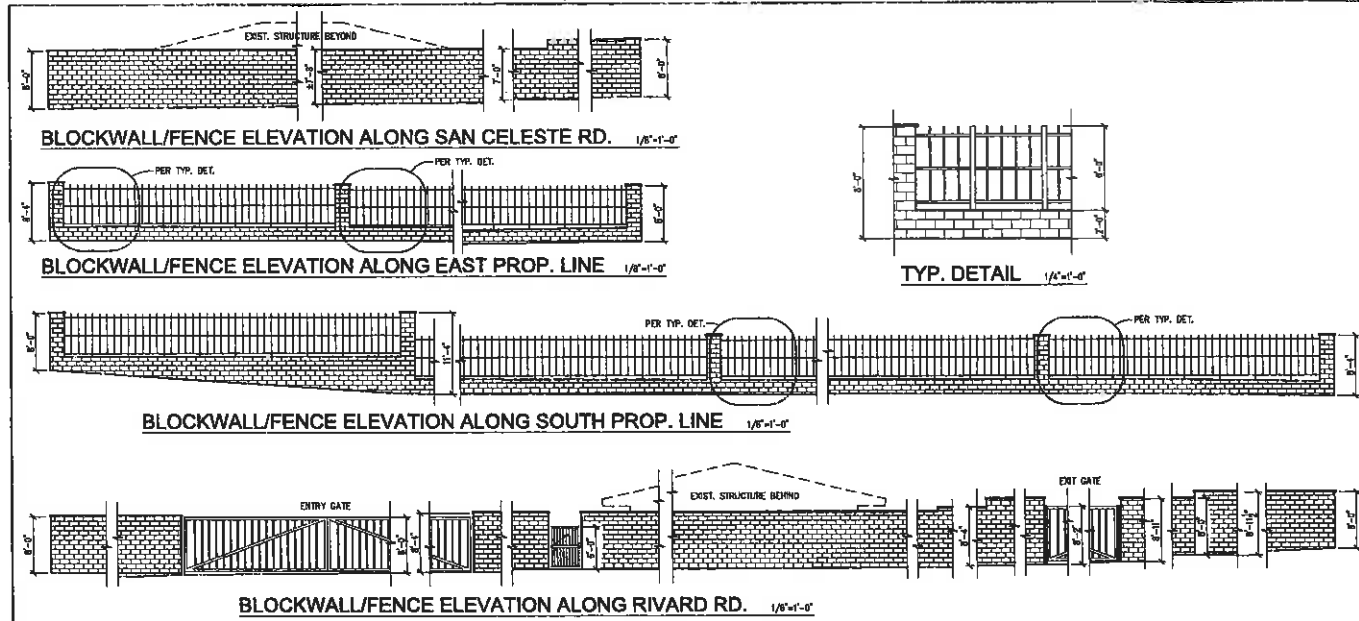


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## Notes



SITE PLAN 1/8"=1'-0"

**PROJECT DATA:**  
 APN: ... AREA = (1 4.63 AC. = 1 289,254 SQ. FT.)  
 ZONE: \_\_\_\_\_  
 CODE USED: HEALTH & SAFETY CODE (HSC) SEC. 19448, CALIFORNIA CODE OF REGULATIONS (CCR), TITLE 8, TITLE 24, 2813 CAC, CBC, CBC, CFC, CFC, CFC, CELVC, CFC, CBC8 & 2813 CALIFORNIA ENERGY CODE.  
 OCCUPANCY: (B) OFFICE  
 OCCUPANCY LOAD: OFFICE .1100 = 309,468 ± 3 OCCP.  
 CONSTRUCTION TYPE: (V-B) OFFICE  
 NUMBER OF STORIES: \_\_\_\_\_  
 BUILDING HEIGHT: \_\_\_\_\_ ± 14 FT.  
 BLDG. AREA = (EXIST. STRUCTURE TO BE AN OFFICE) = 3,934 SQ. FT.  
**PARKING SPACE ANALYSIS:**

Land Use/Building	Parking Ratio	Blgd. Size/Sale	No. of Stall Req'd.
OFFICE	1.824/1780 SF.	3,934 SQ.FT./	13
- Loading Zone	(1) Per Use		1 (11A)
<b>TOTAL PARKING STALL REQUIRED:</b>			<b>14</b>

**PARKING STALL PROVIDED:**

(7) ± STD. 9'x30' STALL	
(1) ± VAN ACCESSIBLE STALL	
(5) ± TRUCK STALL, 14'x30'	
(1) ± LOADING ZONE 9'x30'	
<b>TOTAL PARKING STALL PROVIDED:</b>	<b>14</b>

LOT COVERAGE: 3824/282,254 = 1.35 %  
 HANDICAPPED: 1631/163,254 = 0.99 %  
 LANDSCAPED: 26,588/282,254 = 9.42 %  
 UNDEVELOPED: 9,338/282,254 = 3.31 %

**LEGAL DESCRIPTION:**

**UTILITY INFORMATION:**

- WATER DISTRICT: PONTANA WATER COMPANY, P.O. BOX 5976, EL MONTE, CA 91734 (562) 827-2261
- POWER ELECTRIC: SOUTHERN CALIF. EDISON, P.O. BOX 8629, REDDING, CA 96071 (530) 884-8223
- GAS: SO. CALIF. GAS CO., P.O. BOX 121, MONTEREY, CA 91766 (562) 421-2250
- TELEPHONE: AT&T, P.O. BOX 5625, CAROL STREAM, IL, 60817 (800) 368-2626
- CABLE: AT&T, P.O. BOX 5625, CAROL STREAM, IL, 60817 (800) 368-2626
- WASTE DISPOSAL: CITY OF PONTANA, CITY OF PONTANA, CITY OF PONTANA

**SCOPE OF WORK:**  
 CONVERT AN EXISTING RESIDENTIAL STRUCTURE INTO AN ACCESSIBLE OFFICE BUILDING (A.D.A. COMPLIANCE) AND WITH A PARKING YARD FOR SEMI-TRUCK AND TRAILERS.

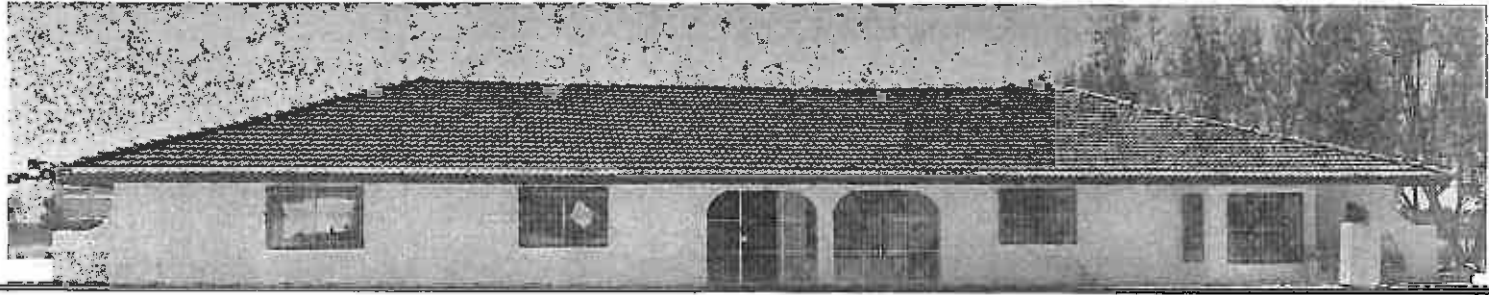
**INLAND ENGINEERS & ASSOCIATES**  
 CIVIL ENGINEERING - PROJECT MANAGEMENT  
 RIVERSIDE, CALIFORNIA 92517  
 951.907.9788

PROJECT: MR. TIMOTHY A. RIVARD  
 SHEET ADDRESS: 8884 JURUPA AVENUE, RIVERSIDE, CA 92509

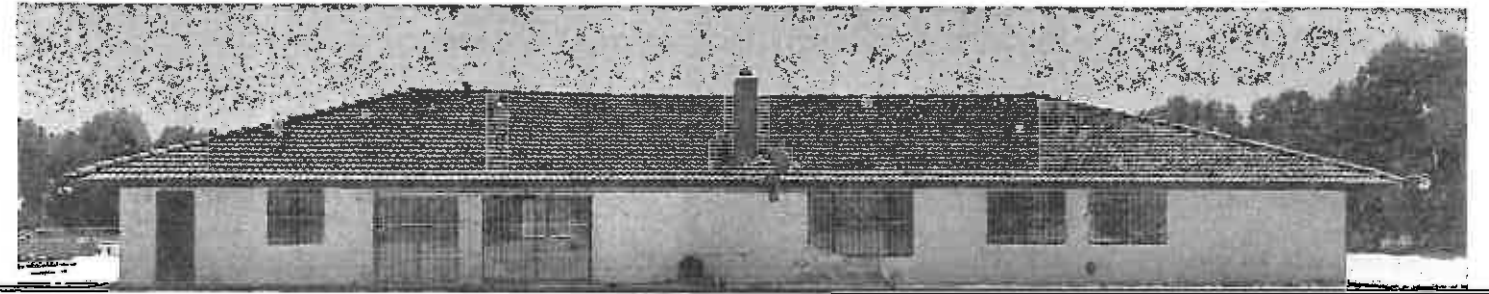
PRODUCT: SITE PLAN  
 PROJECT ADDRESS: 24811 RIVARD RD., ANDREWS VALLEY, CA

DATE: 7-31-17  
 SHEET TITLE: ST-1





EXIST. EAST ELEVATION 1/25/12



EXIST. WEST ELEVATION 1/25/12



EXIST. NORTH ELEVATION 1/25/12



EXIST. SOUTH ELEVATION 1/25/12



# CITY OF MORENO VALLEY PRELIMINARY GRADING PLAN TRUCK STORAGE & OFFICE 24811 RIVARD RD.

**OWNER**

TRACY A. INAND  
2884 JARANA AVENUE  
MORENO VALLEY, CA 92552

**SITE ADDRESS**

24811 INAND RD.  
MORENO VALLEY, CA 92551

**ASSESSOR'S PARCEL NUMBERS**

318-180-012  
318-180-042

**LEGAL DESCRIPTION**

PARCEL 2, SUBDIVISION P.M. 19891, RECORDED P.M. 111/87-24.

**SOURCE OF SURVEY**

JACQUES LEMAY (SUDRA) & ASSOCIATES, INC.  
1248 W 20TH STREET  
SALINA, CA 91774  
CELL (909) 819 0243  
FAX (909) 848 2752

**PROJECT INFORMATION**

SITE AREA -  
4.80 ACRES (204,880 S.F.) GROSS  
4.86 ACRES (212,824 S.F.) NET  
DISTURBED AREA - 194,743 S.F.  
1,110 C.Y. CUT  
1,110 C.Y. FILL

SOIL EXPLORATION COMPANY, INC.  
2335 JARANA AVE., UNIT C  
PERRIS, CA 92504

GENE K. LIAL, P.E. 83417

**SCOPE OF WORK**

PROJECT DESCRIPTION: CONCEPT EXISTING HOUSE TO OFFICE  
BUILDING WITH MODIFICATIONS TO COMPLY WITH ADA REQUIREMENTS,  
LAND FOR SHOPPING/PARKING TRAILER.

**UTILITIES**

TABLE 7A  
SPECTRUM CABLE  
1500 W AUTO CENTER DRIVE  
CERRITOS, CA 91710  
PH (925) 975-3402

GAS  
SOUTHERN CALIFORNIA GAS COMPANY  
32000 TRUMBULL ROAD  
REDDING, CA 96002  
PH (916) 828-3777

SEWER AND WATER  
CANTON METROPOLITAN WATER DISTRICT  
2275 TRUMBULL ROAD  
PERRIS, CA 92577  
PH (951) 958-3777

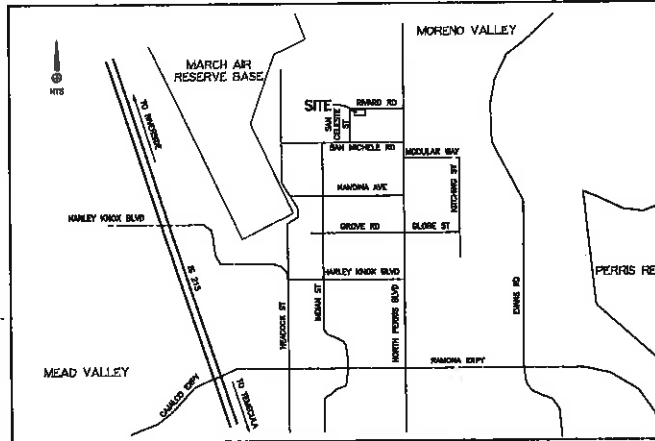
TELEPHONE  
VERIZON  
150 SOUTH ALHAMBRA  
INDENT, CA 92543  
PH (951) 958-8412

ELECTRICITY FOR RELOCATION:  
SOUTHWEST CALIFORNIA Edison  
78100 IMPERIE ROAD  
RIVERSIDE, CA 92509  
PH (951) 825-8333

ELECTRICITY PROVIDER:  
MORENO VALLEY ELECTRIC UTILITY  
14325 PRESIDENT STREET  
MORENO VALLEY, CA 92552  
PH (977) 811-4700

**BASIS OF BEARINGS**

BASIS OF BEARINGS FOR THIS TOPIC IS A CONTINUING  
OF RIVARD ROAD, PER P.M. 30800, P.M. 234/31-33



VICINITY MAP

**GENERAL NOTES**

- 1.) ALL WORK SHALL CONFORM TO THE CITY OF MORENO VALLEY GRADING REGULATIONS, THE ADOPTED CALIFORNIA BUILDING CODE, AND THE LATEST EDITION OF THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION.
- 2.) IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION OF ALL UTILITIES OR STRUCTURES ABOVE OR BELOW GROUND, SHOWN OR NOT SHOWN ON THESE PLANS. THE CONTRACTOR WILL BE HELD RESPONSIBLE FOR ALL DAMAGE TO ANY UTILITIES OR STRUCTURES CAUSED BY HIS/HERS OPERATIONS.
- 3.) ADJACENT STREETS ARE TO BE CLEANED DAILY OF ALL DIRT AND DEBRIS THAT ARE THE RESULT OF OPERATIONS.
- 4.) TRAFFIC SHALL BE CONTROLLED BY METHODS OR OTHER APPROVED METHODS.
- 5.) LOUVERS OF OPERATIONAL AREAS (DOORS, WINDOWS, ETC.) SHALL BE PROTECTED BY PROTECTIVE COVERINGS.
- 6.) ALL OPERATIONS SHALL BE CONDUCTED IN ACCORDANCE WITH THE CITY ENGINEER'S ORDINANCES AND ANY OTHER APPLICABLE ORDINANCES.
- 7.) ALL GRADING SHALL BE COMPLETED UNDER THE SUPERVISION OF A REGISTERED SOIL ENGINEER OF RECORD IN CONFORMANCE WITH RECOMMENDATIONS OF THE PRELIMINARY SOILS INVESTIGATION BY SOIL EXPLORATION COMPANY INC.
- 8.) TWO SETS OF THE FINAL SOILS REPORT SHALL BE SUBMITTED TO THE ENGINEERING DEPARTMENT FOR REVIEW AND APPROVAL PRIOR TO THE ISSUANCE OF A BUILDING PERMIT. THE SOILS REPORT SHALL REFLECT THE FACT THAT THE CONSTRUCTION HAS BEEN OBTAINED NOT ONLY IN THE BUILDING AND LOCATIONS, BUT IN THE REMAINDER OF THE SITE, INCLUDING THE SLOPES. FINAL SOILS REPORTS IDENTIFICATION SHALL BE SUBMITTED BY THE SOILS ENGINEER OF RECORD THAT THE FINAL REPORTS CONFORM TO APPENDIX 4.1 OF THE CALIFORNIA BUILDING CODE (CBC) AND THE APPROVED GRADING PLAN.
- 9.) ALL SLOPES SHALL BE A MINIMUM OF 2:1, CUT OR FILL UNLESS OTHERWISE RECOMMENDED BY REGISTERED SOILS ENGINEER AND APPROVED BY THE CITY ENGINEER.
- 10.) ALL PILES AND BENTON SHALL BE A MINIMUM OF 18" TO STREET OR CURB.
- 11.) ALL TRENCH BACKFILLS SHALL BE TESTED AND CERTIFIED BY THE SOILS ENGINEER OF RECORD TO NOT LESS THAN 80% MAXIMUM DENSITY AS DETERMINED BY A.S.T.M. SOIL COMPRESSION TEST (SPT), THE TOP 12" FT. OF BACKFILL BELOW THE STREET PAVEMENT STRUCTURAL SECTION SHALL BE COMPACTED TO 90% RELATIVE COMPRESSION.
- 12.) SEPARATE PERMITS SHALL BE REQUIRED FOR ANY IMPROVEMENT WORK WITHIN THE PUBLIC RIGHT-OF-WAY.
- 13.) CUT SLOPES GREATER THAN 3 FEET IN VERTICAL HEIGHT AND ALL SLOPES GREATER THAN 3 FEET IN HORIZONTAL HEIGHT SHALL BE PLANTED WITH APPROVED SOILS COVER OR COVERED APPROVED SLOPE EROSION CONTROL METHOD TO PROTECT SLOPE FROM EROSION AND INSTABILITY IN ACCORDANCE WITH THE GRADING REGULATIONS.
- 14.) SEPARATE PERMITS FROM THE BUILDING DEPARTMENT ARE REQUIRED FOR ALL WELLS AND PIPES.
- 15.) ALL SLOPES ADJACENT TO THE PUBLIC RIGHT-OF-WAY SHALL BE SET BACK 2 FEET IF HEIGHT IS LESS THAN 10 FEET, AND 3 FEET IF HEIGHT IS GREATER THAN 10 FEET.
- 16.) DAMAGED OR ALTERED PUBLIC IMPROVEMENTS SHALL BE REPAIRED OR REPLACED AS REQUIRED BY THE CITY ENGINEER.
- 17.) AN "AS-BUILT" GRADING PLAN SHALL BE SUBMITTED AT THE COMPLETION OF WORK AND PRIOR TO THE ISSUANCE OF THE OCCUPANCY PERMIT.
- 18.) IDENTIFICATION BY THE USE OF RECORD THAT THE BROUGH GRADING SOILS COMPARISON HAS BEEN OBTAINED FOR ITEMS 7 AND 11 AND THE SITE CONFORMS TO THIS PLAN AS TO LINE AND GRADE SHALL BE REQUIRED PRIOR TO THE BEGINNING OF BUILDING PERMIT.
- 19.) THE SOILS ENGINEER OF RECORD SHALL BE RESPONSIBLE FOR ASSURING THE ACCURACY AND COMPLETION OF THE DESIGN SECTION. IN THE EVENT OF DISCREPANCIES ARISING DURING CONSTRUCTION, THE PLACE OF RECORD SHALL BE RESPONSIBLE FOR DETERMINING AN ACCEPTABLE SOLUTION AND REVISIONS TO THE PLAN FOR APPROVAL BY THE CITY ENGINEER.
- 20.) ALL IMPORTED SOIL SHALL HAVE A CERTIFICATE GIVEN TO THE CITY ENGINEER STATING THAT THE SOIL IS FREE FROM CONTAMINANTS BEFORE SOIL IS UNCOVERED.
- 21.) HEREBY STATE THAT THIS PLAN WAS PREPARED UNDER MY SUPERVISION AND THAT IT CONFORMS TO THE LATEST EDITION OF THE CALIFORNIA BUILDING CODE (CBC) AS ENFORCED BY CITY OF MORENO VALLEY ORDINANCES, THE INTERIM GUIDELINES, AND THE PRELIMINARY SOILS REPORT PREPARED FOR THIS PROJECT.

NAME: R.E.E. 52789 DATE: \_\_\_\_\_

**PRIVATE ENGINEERS NOTICE TO CONTRACTOR**

THE EXISTENCE AND LOCATION OF ANY UNDERGROUND UTILITY PIPES, CONDUITS, OR STRUCTURES SHOWN ON THESE PLANS WERE OBTAINED BY A SEARCH OF THE AVAILABLE RECORDS. TO THE BEST OF OUR KNOWLEDGE, THERE ARE NO UTILITIES EXCEPT AS SHOWN ON THESE PLANS. THE CONTRACTOR IS REQUIRED TO TAKE SUCH PRECAUTIONARY MEASURES TO PROTECT THE UTILITY LINES SHOWN ON THESE DRAWINGS. THE CONTRACTOR FURTHER ASSUMES ALL LIABILITY AND RESPONSIBILITY FOR THE UTILITY PIPES, CONDUITS, OR STRUCTURES SHOWN OR NOT SHOWN ON THESE DRAWINGS.

THE CONTRACTOR AGREES THAT HE SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THE PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UTILITIES AND STRUCTURES SHOWN ON THESE PLANS AND SHALL BE LIMITED TO NORMAL WORKING HOURS. AND THAT THE CONTRACTOR SHALL OBTAIN NECESSARY AND HOLD THE CITY, THE OWNER, AND THE PRIVATE ENGINEER HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPT FROM THE LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE OWNER OR THE ENGINEER.

**UNAUTHORIZED CHANGES AND USES**

THE ENGINEER PREPARING THESE PLANS WILL NOT BE RESPONSIBLE FOR OR LIABLE FOR UNAUTHORIZED CHANGES TO OR USES OF THESE PLANS. ANY CHANGES TO THE PLANS MUST BE IN WRITING AND MUST BE APPROVED BY THE PREPARER OF THESE PLANS.

**CONSTRUCTION NOTES:**

- 1) PROTECT IN PLACE.
- 2) PLACE 0.50" AC OVER 1.0" CLASS 2 AS TYPICAL SECTION PER CITY STD M&S-8234-1.
- 3) CONSTRUCT 4" CONCRETE TYPE A CURB PER CITY STD M&S-1214-0.
- 4) CONSTRUCT 4" SLOTTED CONCRETE INTERIOR CURB & GUTTER TYPE 6 PER CITY STD M&S-1204-0.
- 5) CONSTRUCT 4" CONCRETE INTERIOR CURB & GUTTER TYPE 6 PER CITY STD M&S-1104-0.
- 6) CONSTRUCT CONCRETE INTERIOR CURB & GUTTER TYPE 6 PER CITY STD M&S-1120-0.
- 7) CONSTRUCT CONCRETE INTERIOR CURB & GUTTER TYPE 6 PER CITY STD M&S-1104-0.
- 8) CONSTRUCT 4" HYDRA WALL UNDER SEPARATE PERMIT.
- 9) CONSTRUCT 3" HYDRA WALL WITH 5" HYDROGRAP HOOP FENCE UNDER SEPARATE PERMIT.
- 10) CONSTRUCT 3" WIDE MEDIAN GUTTER PER DETAIL.
- 11) USE EXIST PAVEMENT PER CITY STANDARD M&S-121-0.

**LEGEND**

- AC = ASPHALT CONCRETE
- BC = BITUM
- BU = BRICK
- BS = BUILDING SIDEWALK LINE
- HW = HATCH OF WALL
- CS = CURB & GUTTER
- CL = CENTER LINE
- EX = EXISTING
- EP = EXIST PAVEMENT
- FC = FINISH CURB
- FL = FLOOR LINE
- FS = FINISH SURFACE
- FM = FINISH MEDIAN
- FP = FINISH PAVEMENT
- FW = FINISH WALL
- GW = GROUND WATER
- HW = HATCH OF WALL
- IS = INTERIOR SURFACE
- SP = SIDEWALK
- TS = TOP OF CURB
- TP = TELEPHONE POLE

- WM = WATER METER
- TEL = TELEPHONE POLE
- GV = GAS VALVE
- DC = DRAINAGE CONTOUR
- PC = PROPOSED CONTOUR

**LEGEND**

- DOTTING CONCRETE PAVING
- EXISTING AC PAVING
- PROPOSED ASPHALT CONCRETE PAVING
- PROPOSED LANDSCAPING (SECTION)
- PROPOSED LANDSCAPING (PLAN)
- ACCESSIBLE PATH OF TRAVEL



SHOULD CONSTRUCTION OF THE REQUIRED IMPROVEMENTS NOT COMMENCE WITHIN TWO YEARS OF THE DATE OF APPROVAL SHOWN HEREON AND CARRIED FORTH IN A DILIGENT MANNER, THE CITY ENGINEER MAY REQUIRE REVISIONS TO THE PLANS TO BRING THEM INTO CONFORMANCE WITH STANDARDS IN EFFECT.



NO.	REVISION	DATE	BY	CHK	APP

**BENCHMARK:**  
REVERSH COUNTY SURVEYOR B.M. NO. "14-52" AT INTERSECTION OF RIVARD & INLAND AVE. 0.500 FT. S/W OF A "CORNER" 10" IN A 2" IRON ROD. POWER POLE #117136 34.30 FT. WEST SIDE OF POWER POLE #117136 34.30 FT. EAST SIDE OF INLAND ST. 10" IRON PIPE & 1/2" IRON ROD. POWER POLE #117136 & 1/2" IRON PIPE & 1/2" IRON ROD. POWER POLE #117136 & 1/2" IRON PIPE & 1/2" IRON ROD. POWER POLE #117136 & 1/2" IRON PIPE & 1/2" IRON ROD.

**INLAND ENGINEERS & ASSOCIATES**  
CIVIL ENGINEERING - PROJECT MANAGEMENT  
P.O. BOX 6486  
MORENO VALLEY, CA 92517  
TEL. (951) 807-6706

PREPARED UNDER THE SUPERVISION OF:

DATE: \_\_\_\_\_

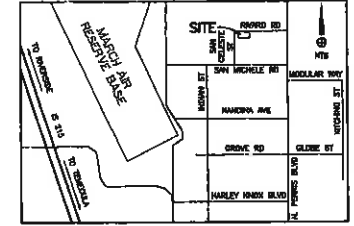
MORENO VALLEY, CALIFORNIA

**CITY OF MORENO VALLEY, CALIFORNIA**  
ON SITE IMPROVEMENTS - GRADING PLAN

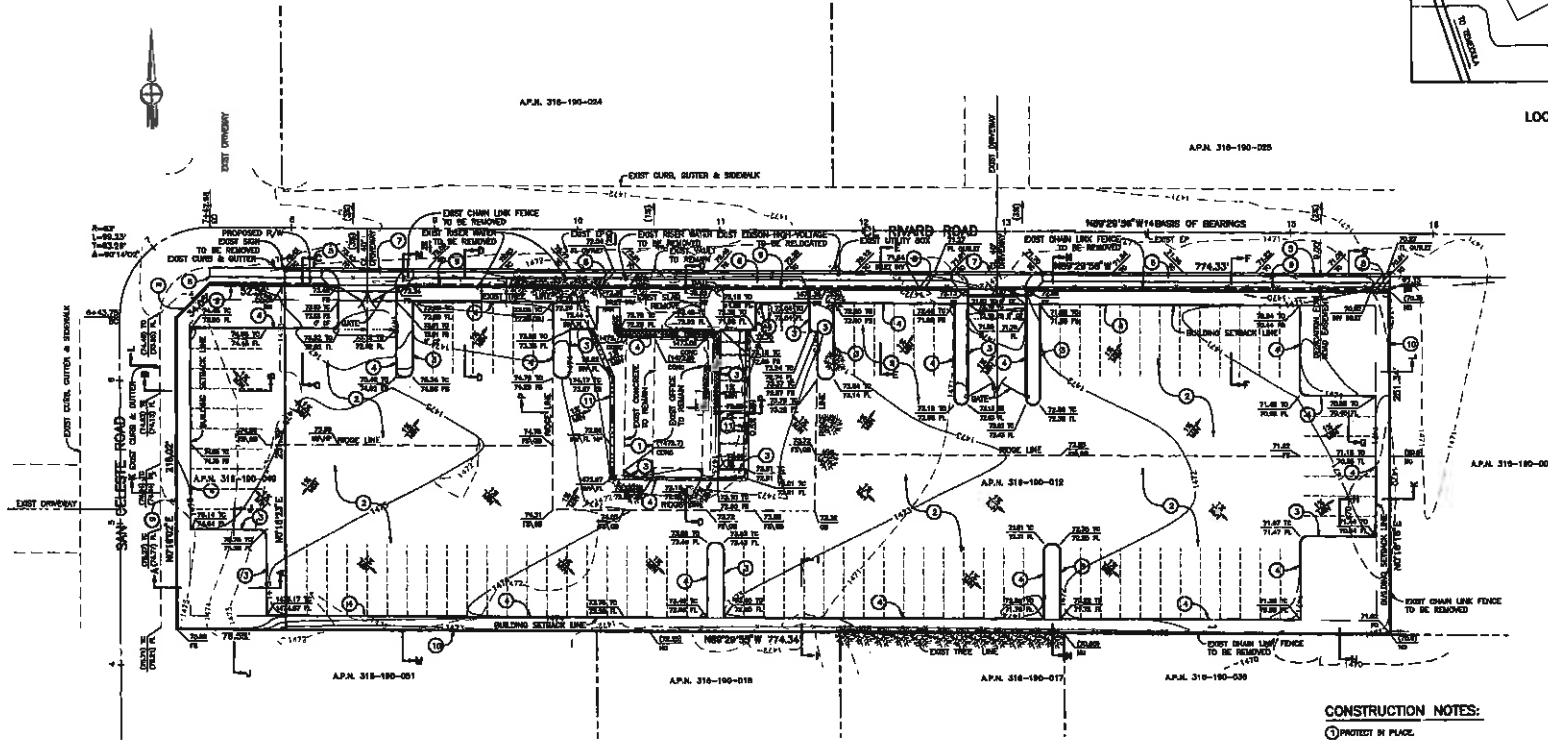
OWNER: TRUCK STORAGE & OFFICE	SCALE: 1"=40'
DESIGNED BY: INLAND ENGINEERS & ASSOCIATES	DATE: 7/30/2017
CHECKED BY: _____	APPROVED BY: _____
DATE: _____	CITY ENGINEER: _____



# CITY OF MORENO VALLEY PRELIMINARY GRADING PLAN TRUCK STORAGE & OFFICE 24811 RIVARD RD.



LOCATION MAP



**CONSTRUCTION NOTES:**

1. PROTECT IN PLACE.
2. PLACE 6.00' AC OVER 1.0' CLASS 3 AS TYPICAL SECTION PER CITY STD MWS-023A-1.
3. CONSTRUCT 4' CONCRETE TYPE A CURB PER CITY STD MWS-1214-0.
4. CONSTRUCT 4' SLOTTED CONCRETE INTERM. CURB & GUTTER TYPE 6 PER CITY STD MWS-1229-0.
5. CONSTRUCT 4' CONCRETE INTERM. CURB & GUTTER TYPE 6 PER CITY STD MWS-1300-0.
6. CONSTRUCT 3' WIDE ASPHALT DRIVEWAY PER CITY STD MWS-118A-0.
7. CONSTRUCT COMMERCIAL DRIVEWAY APPROACH TYPE 3 PER CITY STD MWS-1180-0.
8. CONSTRUCT PARALLEL CULVERT PER CITY STD MWS-150A-0, S-1.
9. CONSTRUCT 8' HT CHAIN WALL UNDER SEPARATE PERMIT.
10. CONSTRUCT 3' HT CHAIN WALL WITH 3' HT BROUGHT IRON FENCE UNDER SEPARATE PERMIT.
11. CONSTRUCT 3' WIDE ASPHALT DRIVEWAY PER DETAIL.
12. JOIN EXIST PAVEMENT PER CITY STANDARD MWS-131-0.

**BASIS OF BEARINGS**

BASIS OF BEARINGS FOR THIS TOPO IS A CENTERLINE OF RIVARD ROAD, PER PH 38800, PH# 335/31-33



SHOULD CONSTRUCTION OF THE REQUIRED IMPROVEMENTS NOT COMMENCE WITHIN TWO YEARS OF THE DATE OF APPROVAL, SHOWN HEREIN AND CARRIED FORTH IN A DILIGENT MANNER, THE CITY ENGINEER MAY REQUIRE REVISIONS TO THE PLANS TO BRING THEM INTO CONFORMANCE WITH STANDARDS IN EFFECT.



NO.	REVISION	DATE	BY	CHK	APP

**BENCHMARK:**  
REYNOLDS COUNTY SURVEY B.M. NO. 74-37 AT INTERSECTION OF PERRY BLVD. & 102 AVE. 5.00 FT. 6/4" OF A CORNER TO A 1" 20' BORN COR. POST. 4.00 FT. 1/4" OF AN 1" 100' IN THE WEST SIDE OF POWER POLE #112136. 34.36 FT. 1/4" OF AN 1" 100' IN THE EAST SIDE TELEPHONE POLE #112182. A 1" IRON PIPE & TAG MARKED COUNTY SURVEYOR IN A HANWELL INSTRUMENT. ELEV. 1004.00' (MWS-38/ESTABLISHED 1963)

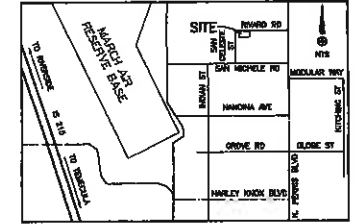
**INLAND ENGINEERS & ASSOCIATES**  
CIVIL ENGINEERING - PROJECT MANAGEMENT  
P.O. BOX 2486  
MORNING, CA 92551  
TEL (951) 907-9768



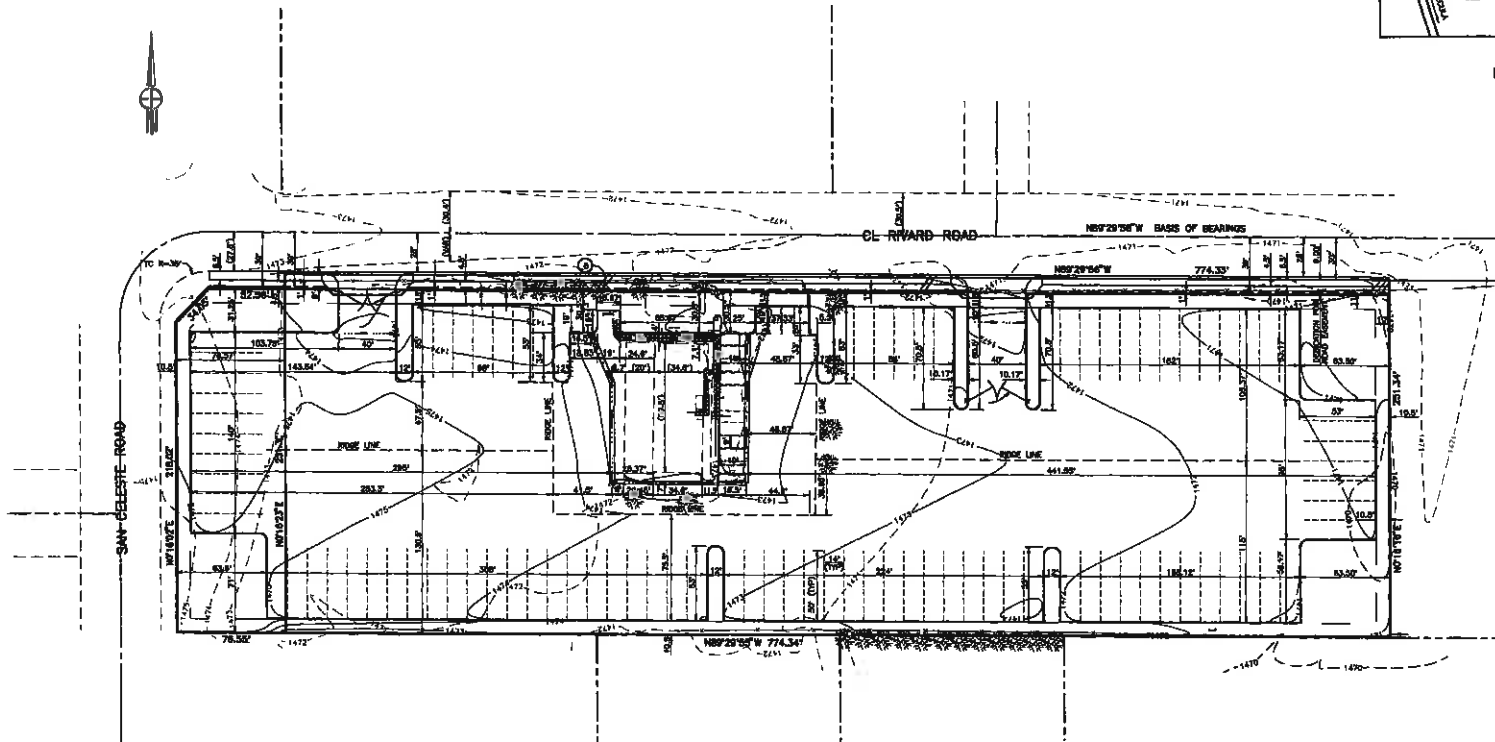
PREPARED UNDER THE SUPERVISION OF:  
MOHAMMAD BODDOKI P.E. 32798

<b>CITY OF MORENO VALLEY, CALIFORNIA</b>	
<b>ON SITE IMPROVEMENTS - GRADING PLAN</b>	
DRAWN BY: WCH CHECKED BY: WCH DATE: 7/20/2017	TRUCK STORAGE & OFFICE 24811 RIVARD RD. MORENO VALLEY, CA 92551 APPROVED BY: [Signature] CITY ENGINEER
SHEET NO. 2 OF 5	DATE: 7/20/2017

# CITY OF MORENO VALLEY PRELIMINARY GRADING PLAN TRUCK STORAGE & OFFICE 24811 RIVARD RD.



LOCATION MAP



**BASIS OF BEARINGS**

BASIS OF BEARINGS FOR THIS TOPO IS A CENTERLINE OF RIVARD ROAD, PER PA 30660, PAR 238/91-33



SHOULD CONSTRUCTION OF THE REQUIRED IMPROVEMENTS NOT COMMENCE WITHIN TWO YEARS OF THE DATE OF APPROVAL, SHOWN HEREON AND CARRIED FORTH IN A DULCIBT MANNER, THE CITY ENGINEER MAY REQUIRE REVISIONS TO THE PLANS TO BRING THEM INTO CONFORMANCE WITH STANDARDS IN EFFECT.



NO.	REVISION	DATE	BY	CHK

**BENCHMARK:**  
RIVERSIDE COUNTY BURNING B.L. NO. "A-37" AT INTERSECTION OF RIVARD BLVD. & 1ST AVE. 54.00 FT. S.W. OF A CORNER "X" IN A 2" IRON CIRC. PIPE - 4.00 FT. WIDE BY 10.00 FT. HIGH IN THE WEST SIDE OF POWER POLE 2011336 34.30 FT. N.W. OF A CORNER "X" IN A 2" IRON PIPE & THE MARKED COUNTY SURVEYOR IN A HANWELL MONUMENT.  
ELEV. 1305.250' (WHD '88/ESTABLISHED 1983)

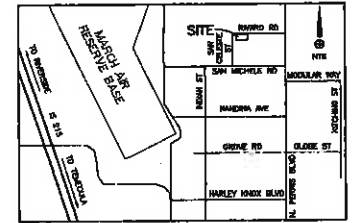
**INLAND ENGINEERS & ASSOCIATES**  
CIVIL ENGINEERING - PROJECT MANAGEMENT  
P.O. BOX 8498  
RIVERSIDE, CA 92517  
TEL. (951) 507-5796



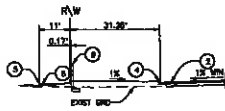
PREPARED UNDER THE SUPERVISION OF:  
DATE: \_\_\_\_\_  
APPROVED BY: \_\_\_\_\_  
CITY ENGINEER

<b>CITY OF MORENO VALLEY, CALIFORNIA</b>	
<b>ON SITE IMPROVEMENTS - HORIZONTAL CONTROL</b>	
DESIGNED BY: _____	DATE: _____
CHECKED BY: _____	APPROVED BY: _____
PROJECT NO.: _____	DATE: 7/30/2017
SCALE: 1"=40'	PROJECT NO.: 15

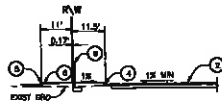
# CITY OF MORENO VALLEY PRELIMINARY GRADING PLAN TRUCK STORAGE & OFFICE 24811 RIVARD RD.



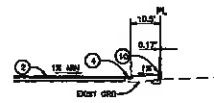
LOCATION MAP



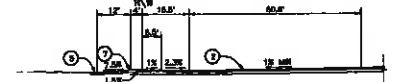
SECTION C-C  
SCALE 1"=20'



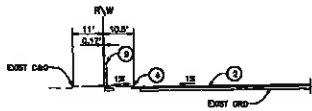
SECTION F-F  
SCALE 1"=20'



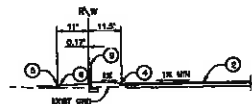
SECTION I-I  
SCALE 1"=20'



PROFILE D/W @ 12+94.35  
SCALE 1"=20'



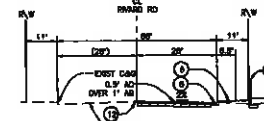
SECTION B-B  
SCALE 1"=20'



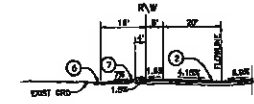
SECTION E-E  
SCALE 1"=20'



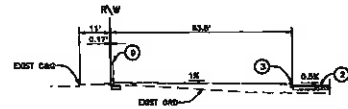
SECTION H-H  
SCALE 1"=20'



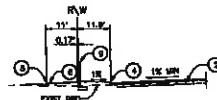
TYPICAL SECTION  
SCALE 1"=20'



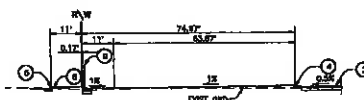
PROFILE D/W @ 8+53.20  
SCALE 1"=20'



SECTION A-A  
SCALE 1"=20'



SECTION D-D  
SCALE 1"=20'



SECTION G-G  
SCALE 1"=20'



SECTION J-J  
SCALE 1"=20'

**BASIS OF BEARINGS**

BASIS OF BEARINGS FOR THIS TOPO IS A CENTERLINE OF RIVARD ROAD, PER PM 35500, PWB 236/31-55

**CONSTRUCTION NOTES:**

1. PROTECT IN PLACE.
2. PLACE 0.50' AC OVER 1.5" CLASS 2 AS TYPICAL SECTION PER CITY STD MWS-1210-1.
3. CONSTRUCT 1" CONCRETE TYPE 4 CURB PER CITY STD MWS-1210-0.
4. CONSTRUCT 1" SLITTED CONCRETE INTERNAL CURB & OUTER TYPE 4 PER CITY STD MWS-1200-0.
5. CONSTRUCT 1" CONCRETE INTERNAL CURB & OUTER TYPE 4 PER CITY STD MWS-1200-0.
6. CONSTRUCT SIDEWALK PER CITY STD MWS-1150-0.
7. CONSTRUCT COMMERCIAL DRIVEWAY APPROACH TYPE 3 PER CITY STD MWS-1150-0.
8. CONSTRUCT PARALLEL DRIVEWAY PER CITY STD MWS-1200-0.
9. CONSTRUCT 3" HT CHU WALL UNDER SEPARATE PERMIT.
10. CONSTRUCT 3" HT CHU WALL WITH 12" HT BROWCAST BENCH UNDER SEPARATE PERMIT.
11. CONSTRUCT 3" WIDE PAVEMENT CUTTER PER DESIGN.
12. JOIN EXIST PAVEMENT PER CITY STANDARD MWS-151-0.



SHOULD CONSTRUCTION OF THE REQUIRED IMPROVEMENTS NOT COMMENCE WITHIN TWO YEARS OF THE DATE OF APPROVAL SHOWN HEREON AND CARRIED FORTH IN A DULCET MANNER, THE CITY ENGINEER MAY REQUIRE REVISIONS TO THE PLANS TO BRING THEM INTO CONFORMANCE WITH STANDARDS IN EFFECT.



NO.	REVISION	DATE	BY	CHK	APP

**BENCHMARK:**  
INDEPENDENT COUNTY SURVEYOR B.M. NO. "4-27" AT INTERSECTION OF PIERCE BLVD. & 9th AVE. 56.25 FT 5/8" OF A CHECKED 12" IN A 3" HIGH CON. POST. 50.25 FT. 1/2" OF 1/2" DIA. & 1/2" IN THE WEST SIDE OF POWER POLE #113138. 34.30 FT. 1/4" OF A 1" DIA. & 1/2" SET BY 1/4" SIDE TELEPHONE POLE #19190. 1" FROM PIPE & TAP W/ROUND COUNTY SURVEYOR IN A MARSHALL MONUMENT.  
ELEV. 1553.520' (MVD) 28/ESTABLISHED 1940

**INLAND ENGINEERS & ASSOCIATES**  
CIVIL ENGINEERING - PROJECT MANAGEMENT  
P.O. BOX 9458  
RIVERSIDE CA 92517  
TEL. (951) 937-9708

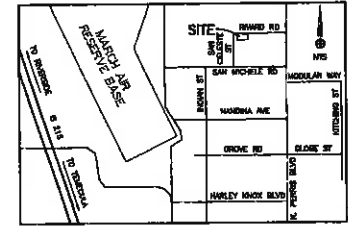


PREPARED UNDER THE SUPERVISION OF:  
DATE: \_\_\_\_\_  
APPROVED BY: \_\_\_\_\_  
CITY ENGINEER

**CITY OF MORENO VALLEY, CALIFORNIA**  
ON SITE IMPROVEMENTS - SECTIONS & D/W PROFILES

DRAWN BY: MCH	TRUCK STORAGE & OFFICE	NO. AS SHOWN
DESIGNED BY: MCH	24811 RIVARD RD.	NO. 7/30/2017
CHECKED BY: MCH	MORENO VALLEY, CA 92551	NO. 4
DATE: _____	APPROVED BY: _____	NO. 5
NO. 1	CITY ENGINEER	

# CITY OF MORENO VALLEY PRELIMINARY GRADING PLAN TRUCK STORAGE & OFFICE 24811 RIVARD RD.

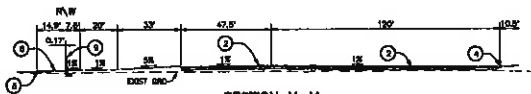


LOCATION MAP



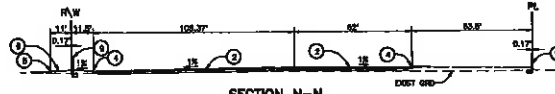
SECTION P-P

SCALE 1"=30'



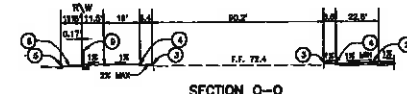
SECTION M-M

SCALE 1"=30'



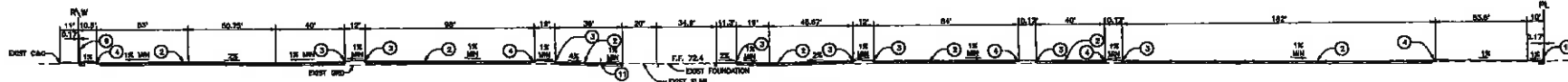
SECTION N-N

SCALE 1"=30'



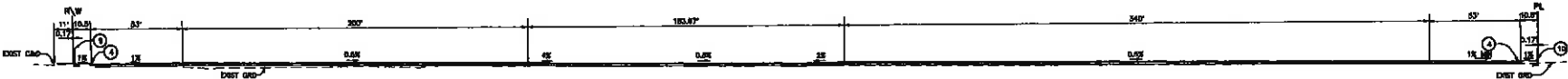
SECTION O-O

SCALE 1"=30'



SECTION I-I

SCALE 1"=30'



SECTION K-K

SCALE 1"=30'

**CONSTRUCTION NOTES:**

1. PROTECT IN PLACE.
2. PLACE CLAY AC OVER 1.5" CLASS 2 AB TYPICAL SECTION PER CITY STD MW-1023A-1.
3. CONSTRUCT 4" CONCRETE TYPE A CURB PER CITY STD MW-1214-0.
4. CONSTRUCT 6" SLOTTED CONCRETE INTERNAL CURB & GUTTER TYPE 6 PER CITY STD MW-1204-0.
5. CONSTRUCT 6" CONCRETE INTERNAL CURB & GUTTER TYPE 6 PER CITY STD MW-1208-0.
6. CONSTRUCT 6" CONCRETE INTERNAL CURB & GUTTER TYPE 6 PER CITY STD MW-1116-0.
7. CONSTRUCT CONCRETE DRIVEWAY APPROACH TYPE 3 PER CITY STD MW-1125-0.
8. CONSTRUCT PAVEMENT CLEVERLY PER CITY STD MW-1004-0, 5-7.
9. CONSTRUCT 6" MC GULF WALL UNDER SEPARATE PERMIT.
10. CONSTRUCT 3' HT GULF WALL WITH 3' HT WROUGHT IRON FENCE UNDER SEPARATE PERMIT.
11. CONSTRUCT 3' WIDE RESSON BUTTER PER DETAIL.
12. JOIN EXIST PAVEMENT PER CITY STANDARD MW-131-0.



**BASIS OF BEARINGS**

BASIS OF BEARINGS FOR THIS TOPIC IS A CENTRAL LINE OF RIVARD ROAD, PER PW 33505, PWS 236/31-33

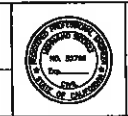
SHOULD CONSTRUCTION OF THE REQUIRED IMPROVEMENTS NOT COMMENCE WITHIN TWO YEARS OF THE DATE OF APPROVAL SHOWN HEREON AND CARRIED FORTH IN A DILIGENT MANNER, THE CITY ENGINEER MAY REQUIRE REVISIONS TO THE PLANS TO BRING THEM INTO CONFORMANCE WITH STANDARDS IN EFFECT.



NO.	NUMBER DESCRIPTION	DATE	BY	CHK.

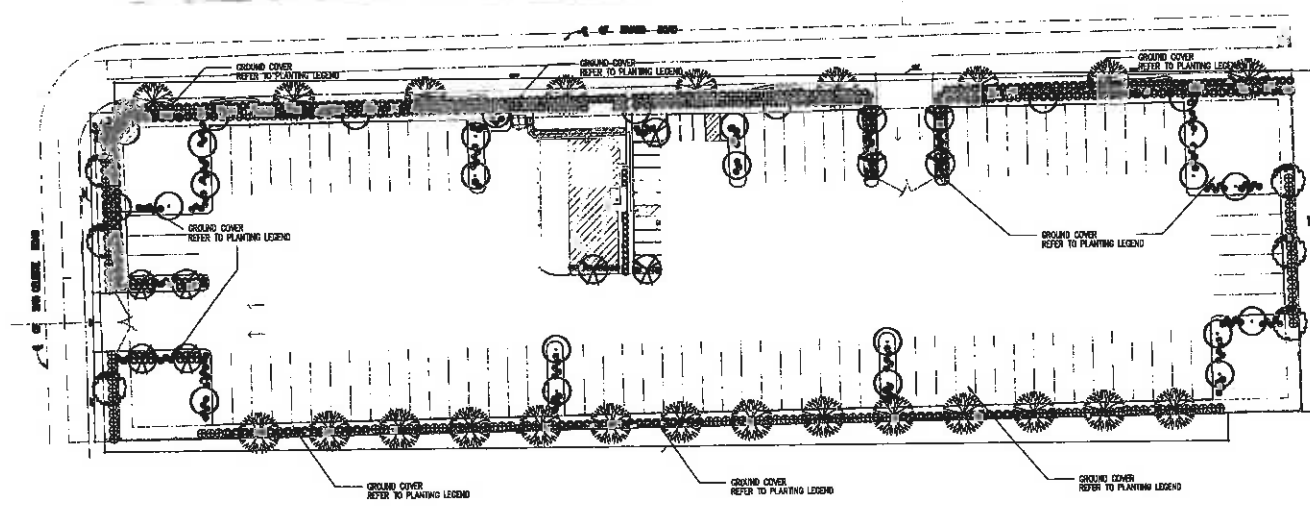
**BENCHMARK:**  
SANBORN COUNTY SURVEYOR BAL NO. 74-32 AT INTERSECTION OF PIONEER BLVD & HWY 91. 34.28 FT 5/8" OF A DIAMETER 12" IN A 2" FROM CORNER POST. 10.5 FT 1/2" OF DIA. & 1/2" IN THE WEST SIDE OF POWER POLE 2117136 34.33 FT. 1/4" OF A DIA. & 1/2" SET IN 5/8" SIDE TELEPHONE POLE #15182. A 1" IRON PIPE AT THE MARKED COUNTY SURVEYOR IN A HANDBOOK BOUNDARY. ELEV. 1505.829' (NOV 28/ESTABLISHED 1962)

**INLAND ENGINEERS & ASSOCIATES**  
CIVIL ENGINEERING - PROJECT MANAGEMENT  
P.O. BOX 8498  
RIVERSIDE, CA 92517  
TEL. (951) 507-8700



PREPARED UNDER THE SUPERVISION OF:  
STEPHEN M. STROH P.E., 82796  
DATE: 7/30/2017

CITY OF MORENO VALLEY, CALIFORNIA		NO.	AS SHOWN
ON SITE IMPROVEMENTS - SECTIONS		DATE	BY
DESIGNED BY	TRUCK STORAGE & OFFICE	7/30/2017	AS SHOWN
CHECKED BY	24811 RIVARD RD.		
APPROVED BY	MORENO VALLEY, CA 92551		
DATE			
ISS	CITY ENGINEER		

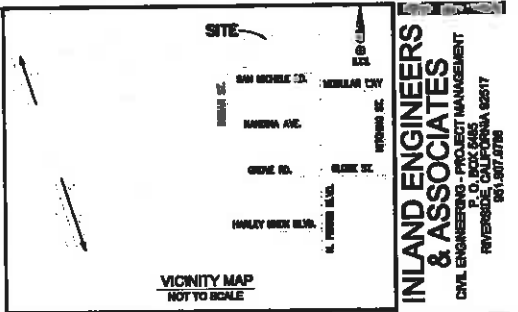


### PLANTING LEGEND

SYMBOL	SCIENTIFIC NAME	COMMON NAME	HEIGHT & SPREAD	QUANTITY	REMARKS	SYMBOL	SCIENTIFIC NAME	COMMON NAME	HEIGHT & SPREAD	QUANTITY	REMARKS
○	LACINIASTRUM INDICA & PALMIE MURDOCKE	MUSKOGEE GRAPE HYDRILE	15' - 20' H UP TO 15' S	-	-	⊙	ASAVE AMERICANA VAR. MENDO-PICATA 'TINY' ALBA	SHADE WHITE-STRIPED CENTURY PLANT	UP TO 3' H UP TO 4' S	-	-
⊙	FIRUS ELDNICA	FINUS BRUTA	60' - 80' H 20' - 30' S	-	-	⊕	DASTURON WHEELER	SOYOL SPOON MOCCA	3' - 4' H 3' - 4' S	-	-
⊙	PODOCARPUS GRACILIOR	FERN PODOCARPUS	40' - 50' H UP TO 25' S	-	-	⊕	DODONAEA VISCOSA 'PURPUREA'	PURPLE HOPBUSH	UP TO 12' H 4' - 6' S	-	-
⊙	ARECATHA ROMANOFFIANUM	QUEEK PALM	35' - 30' H 10' - 12' S	-	-	⊙	HESPERALOE PARVIFLORA	RED YUCCA	2' - 3' H 3' - 4' S	-	-
⊙	MAGNOLIA GRANDIFLORA 'LITTLE GEM'	LITTLE GEM 'DWARF SOUTHERN MAGNOLIA'	20' - 25' H 10' - 15' S	-	-	⊕	LAGUNARIA J. TEANUM	WAX LEAF PRINET	8' - 10' H 4' - 6' S	-	-
⊙						⊕	PHODIA FRAXEIB	FRASER'S PHODIA	8' - 10' H 8' - 10' S	-	-
⊙						⊕	XYLISMA COMESTRUM	SHINY XYLISMA	10' - 12' H 10' - 12' S	-	-
⊙						⊕	LONGERA JAPONICA 'MALLARI'	HALL'S JAPANESE HONEYSUCKLE	UP TO 2' H UP TO 15' S	-	-

BEEM - VARIOUS SIZE AND SHAPE LOCATED INTERMITTENTLY ALONG RIVARD RD. AND SAN CELESTE RD. WITH VARYING HEIGHTS WHICH DO NOT EXCEED 2' HIGH.

PROVIDE 3" THICK MATCH ON ALL NON-TOP SOIL SURFACES



### PROJECT DATA:

APN: 308-190-012 ... AREA = (±) 4.85 AC. = 202,554 SQ. FT.

ZONE: M-1

CODE USED: HEALTH & SAFETY CODE (HSC) SEC. 1304A, CALIFORNIA CODE OF REGULATIONS (CCR) TITLE 19, TITLE 24, 2013 CFC, CFC REG. CFC, CFC CAL. GEN. CFC, CCRS & 2013 CALIFORNIA ENERGY CODE

OCCUPANCY: (B) OFFICE

OCCUPANCY LOAD: OFFICE = 1/100 = 3,034/100 = 31 COO.SF.

CONSTRUCTION TYPE: (V-S) OFFICE

NUMBER OF STORES: 1

BUILDING HEIGHT: 14 FT.

BLDG. AREA = 1) EXIST. STRUCTURE TO BE AN OFFICE = 3,034 SQ. FT.

#### PARKING SPACE ANALYSIS:

Land Use/Req'd	Parking Rate	Bldg. Size/Rate	Stall Req'd
OFFICE	1 Stall / 250 S.F.	3,034 SQ.F.T.	13
- Loading Zone	(1) / Per Use	-	= 1 (Min.)
<b>TOTAL PARKING STALL REQUIRED:</b>			<b>14</b>
<b>PARKING STALL PROVIDED:</b>			(12 = STD. 6'x10' STALL) (1 = VAN ACCESSIBLE STALL) (1 = LOADING ZONE, 10'x20') (80 = TRUCK STALL, 14'x30')
<b>TOTAL PARKING STALL PROVIDED:</b>			<b>103</b>

LOT COVERAGE: 3,034 / 202,554 = 1.49 %

HARDSCAPED: 163,176 / 202,554 = 75.69 %

LANDSCAPED: 34,866 / 202,554 = 17.26 %

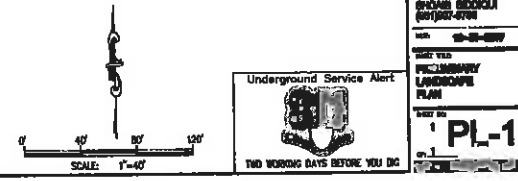
UNTOUCHED: 8,358 / 202,554 = 4.13 %

#### LEGAL DESCRIPTION:

THE NORTH 1/2 OF THE WEST 1/2 OF THE WEST 1/2 OF LOT 83A, ACCORDING TO MAP SHOWING SUBDIVISION OF LANDS BELONGING TO THE SEMI-TROPIC LAND AND WATER COMPANY, IN THE COUNTY OF SAN BERNARDINO, STATE OF CALIFORNIA, AS PER PLAT RECORDED IN BOOK 11, OF MAPS, PAGE 12, RECORDS OF SAID COUNTY.

#### SCOPE OF WORK:

PROVIDE A LANDSCAPE DESIGN THAT WILL ENHANCE BOTH RIVARD AND SAN CELESTE ROADS. THE PLANTS USED WILL NOT ONLY SCREEN A 14' H WALL BUT IT WILL ALSO SERVE AS A PART OF THE STREET DESIGN AND NEW CONCRETE. THE LANDSCAPE DESIGN WILL ALSO SOFTEN THE LOOK OF THE WALL AT THE SAME TIME CARRY THE THEME CONTINUITY WITH SURROUNDING STREETS AND ROADS.



## INLAND ENGINEERS & ASSOCIATES

CIVIL ENGINEERING - PROJECT MANAGEMENT  
LANDSCAPE ARCHITECTURE  
REGISTERED PROFESSIONAL ENGINEERS  
REGISTERED PROFESSIONAL ARCHITECTS  
1000 UNIVERSITY AVENUE  
SUITE 200  
MENDOCINO, CALIFORNIA 95501  
TEL: 707.477.0778

PROJECT: MR. TIMOTHY A. RIVARD  
OWNER: MR. TIMOTHY A. RIVARD  
14851 RIVARD RD.  
MENDOCINO VALLEY, CA 95501

DATE PLANNED: 08/15/2013  
DATE REVISION: 08/15/2013  
DRAWN BY: [Signature]  
CHECKED BY: [Signature]  
SCALE: 1"=40'

**PL-1**

PAGE BREAK





# AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

November 9, 2016

Ms. Judy Eguez, Project Planner

City of Riverside Community and Economic Development Department/Planning Division  
3900 Main Street, Third Floor  
Riverside, CA 92522

**CHAIR**  
Rod Ballance  
Riverside

**VICE CHAIRMAN**  
Steve Manos  
Lake Elsinore

**COMMISSIONERS**

Arthur Butler  
Riverside

John Lyon  
Riverside

Glen Holmes  
Hemet

Russell Betts  
Desert Hot Springs

Steven Stewart  
Palm Springs

**STAFF**

**Director**  
Simon A. Housman

John Guerin  
Paul Rull  
Barbara Santos

County Administrative Center  
4080 Lemon St., 14th Floor.  
Riverside, CA 92501  
(951) 955-5132

[www.rcaluc.org](http://www.rcaluc.org)

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –  
DIRECTOR'S DETERMINATION**

File No.: ZAP1291MA17  
Related File No.: P17-0584 (Design Review)  
APN: 276-160-015

Dear Ms. Eguez:

As authorized by the Riverside County Airport Land Use Commission (ALUC) pursuant to a specific delegation of authority issued at its November 9, 2017 regular meeting, as ALUC Director, I have reviewed City of Riverside Case No. P17-0584 (Design Review), a proposal to construct a gated access parking lot with 114 parking stalls on 2.38 acres located along the southerly side of Lindbergh Drive/Grumman Drive, at the southerly terminus of Earhart Way, within the portion of the Mission Grove neighborhood located southerly of Mission Grove Parkway and easterly of Trautwein Road.

The site is located within Airport Compatibility Zone C2 of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone C2 of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, non-residential intensity is restricted to an average of 200 people per acre and a maximum of 500 people in any given single acre. Based on the number of parking spaces, the anticipated occupancy would be less than 100 persons per acre.

The elevation of Runway 14-32 at March Air Reserve Base/Inland Port Airport at its southerly terminus is approximately 1,535 feet above mean sea level (1535 AMSL). The northerly terminus of this runway is located approximately 16,500 feet from the project site. At this distance, Federal Aviation Administration (FAA) review would be required for any structures exceeding 1,700 feet AMSL. The project site has an elevation of 1,606 feet AMSL. The proposed parking lot light poles will be 20 feet in height, resulting in a top point elevation of 1,626 feet AMSL. Therefore, FAA obstruction evaluation review for height elevation reasons is not required.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, provided that the City of Riverside applies the following recommended conditions:

**CONDITIONS:**

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be

## AIRPORT LAND USE COMMISSION

downward facing.

2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area.
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Any other hazards to flight.
3. The landowner shall provide the attached notice to potential purchasers and lessees of the property. Additionally, this notice shall be recorded as a deed notice.
4. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment of irrigation controllers, access gates, etc.
5. No solar panels are proposed through this action. Any proposal to add solar panel canopies shall be subject to airport land use compatibility evaluation, which shall include a glare study.

If you have any questions, please contact Paul Rull, ALUC Urban Regional Planner IV, at (951) 955-6893 or John Guerin, ALUC Principal Planner, at (951) 955-0982.

Sincerely,  
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Simon A. Housman, ALUC Director

Attachments: Notice of Airport in Vicinity



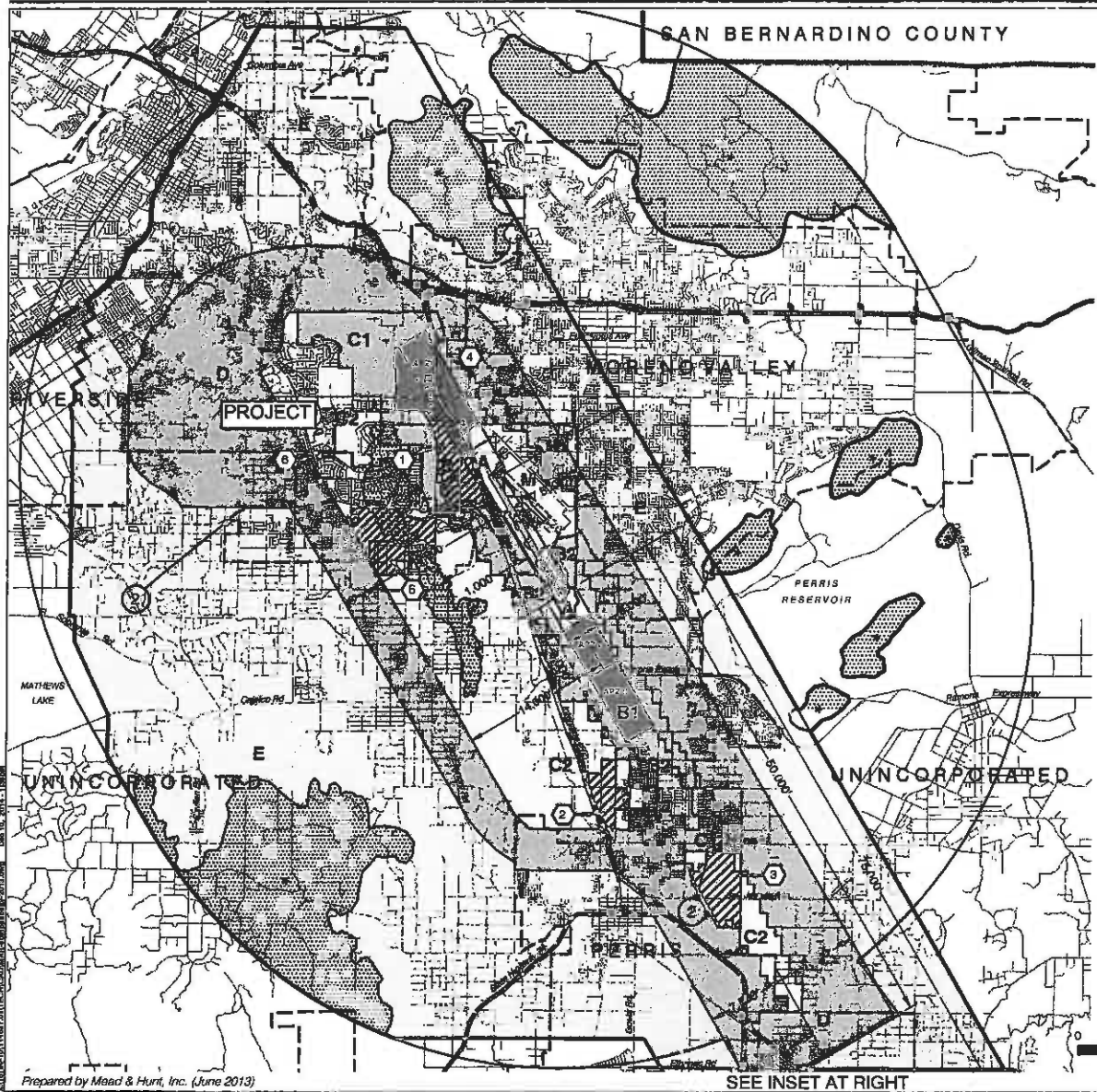
## **AIRPORT LAND USE COMMISSION**

cc: Rick Engineering Group, Kristin Werksman (representative)  
Regional Properties, Inc., Michelle Rubin (applicant/property owner)  
Gary Gosliga, Airport Manager, March Inland Port Airport Authority  
Daniel Rockholt or Denise Hauser, March Air Reserve Base  
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1291MA17\ZAP1291MA17.LTR.doc

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



**LEGEND**

**Compatibility Zones**

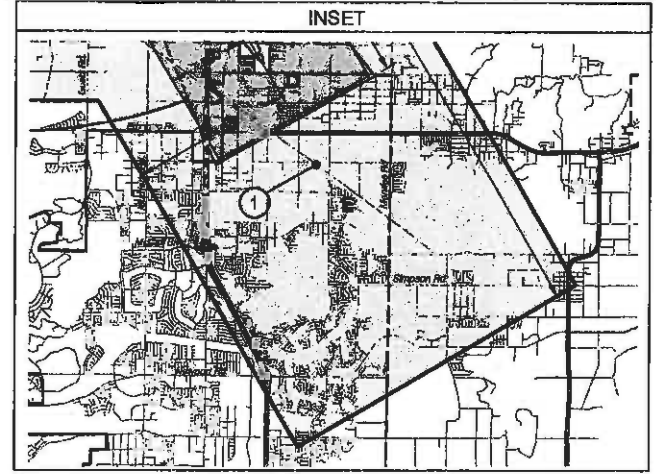
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

- ① Point at which aircraft on Runway 32 (LS) approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

**Boundary Lines**

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- ▭ Site-Specific Exceptions (existing local agency commitments to development projects)

- ① March JPA; March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA; Ban Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



**Riverside County  
Airport Land Use Commission  
March Air Reserve Base / Inland Port Airport  
Land Use Compatibility Plan  
(Adopted November 13, 2014)**

**Note:**  
All dimensions are measured from runway ends and centerlines.



Base map source: County of Riverside 2013

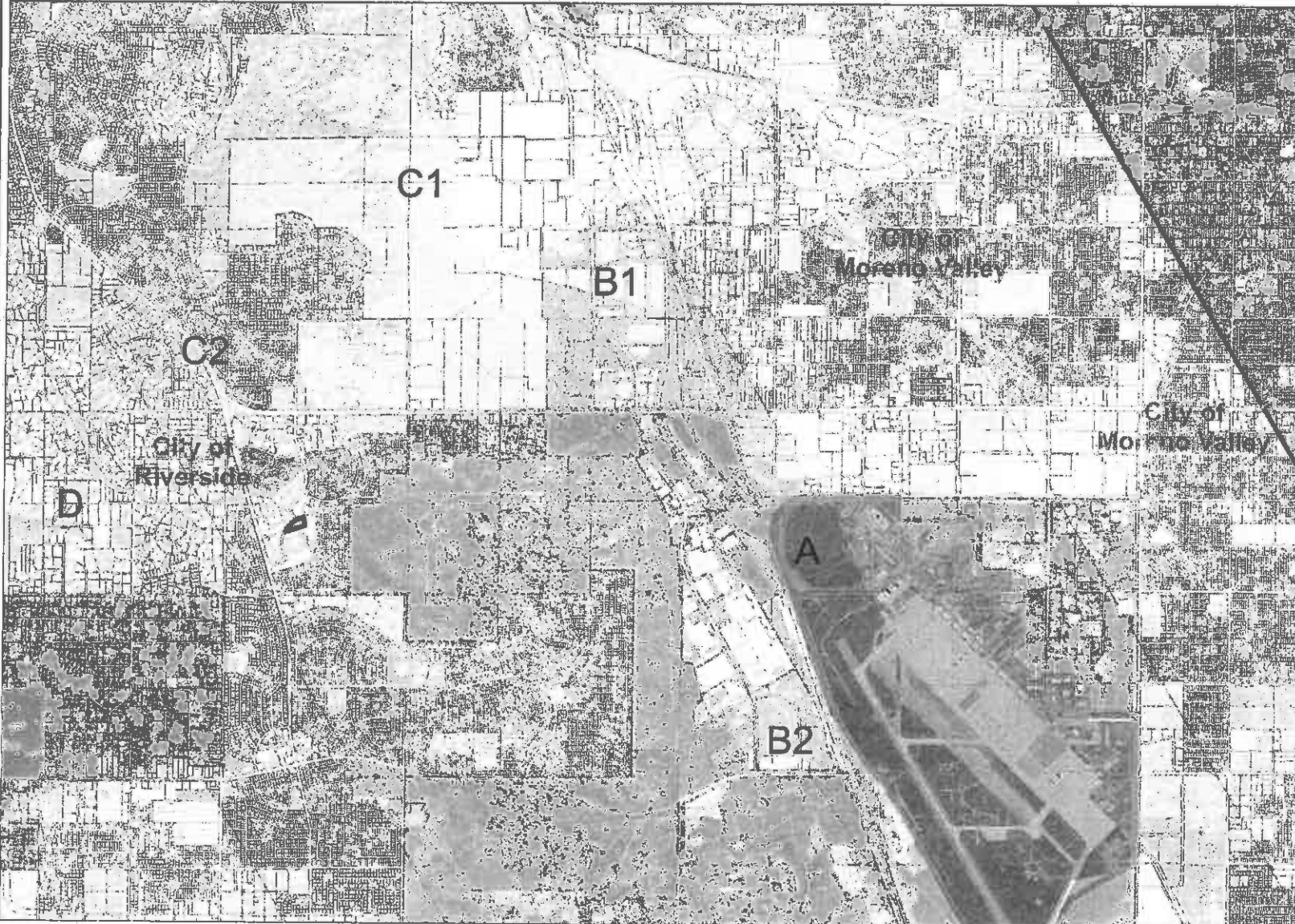
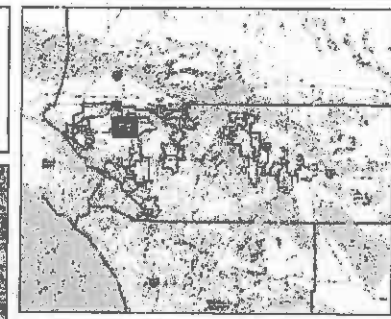
Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map MA-1

**Compatibility Map**  
March Air Reserve Base / Inland Port Airport

# My Map



## Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



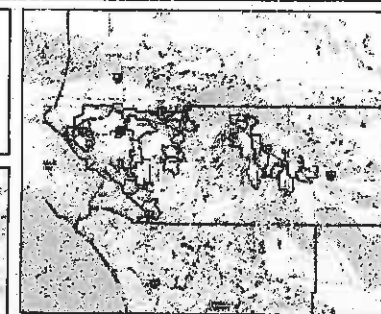
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## Notes

# My Map



- Legend**
- Airports
  - AIA
  - Airport Compatibility**
  - OTHER ZONE
  - A
  - A-EXC1
  - B1
  - B1-APZ I
  - B1-APZ I-EXC1
  - B1-APZ II
  - B1-APZ II-EXC1
  - B1-EXC1
  - B2
  - B2-EXC1
  - C
  - C1
  - C1-EXC1
  - C1-EXC3
  - C1-EXC4
  - C1-HIGHT
  - C2
  - C2-EXC1
  - C2-EXC2
  - C2-EXC3
  - C2-EXC5
  - C2-EXC6
  - C2-HIGHT



0 678 1,356 Feet



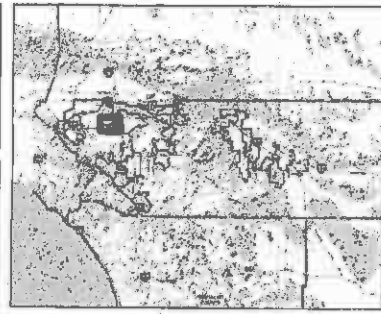
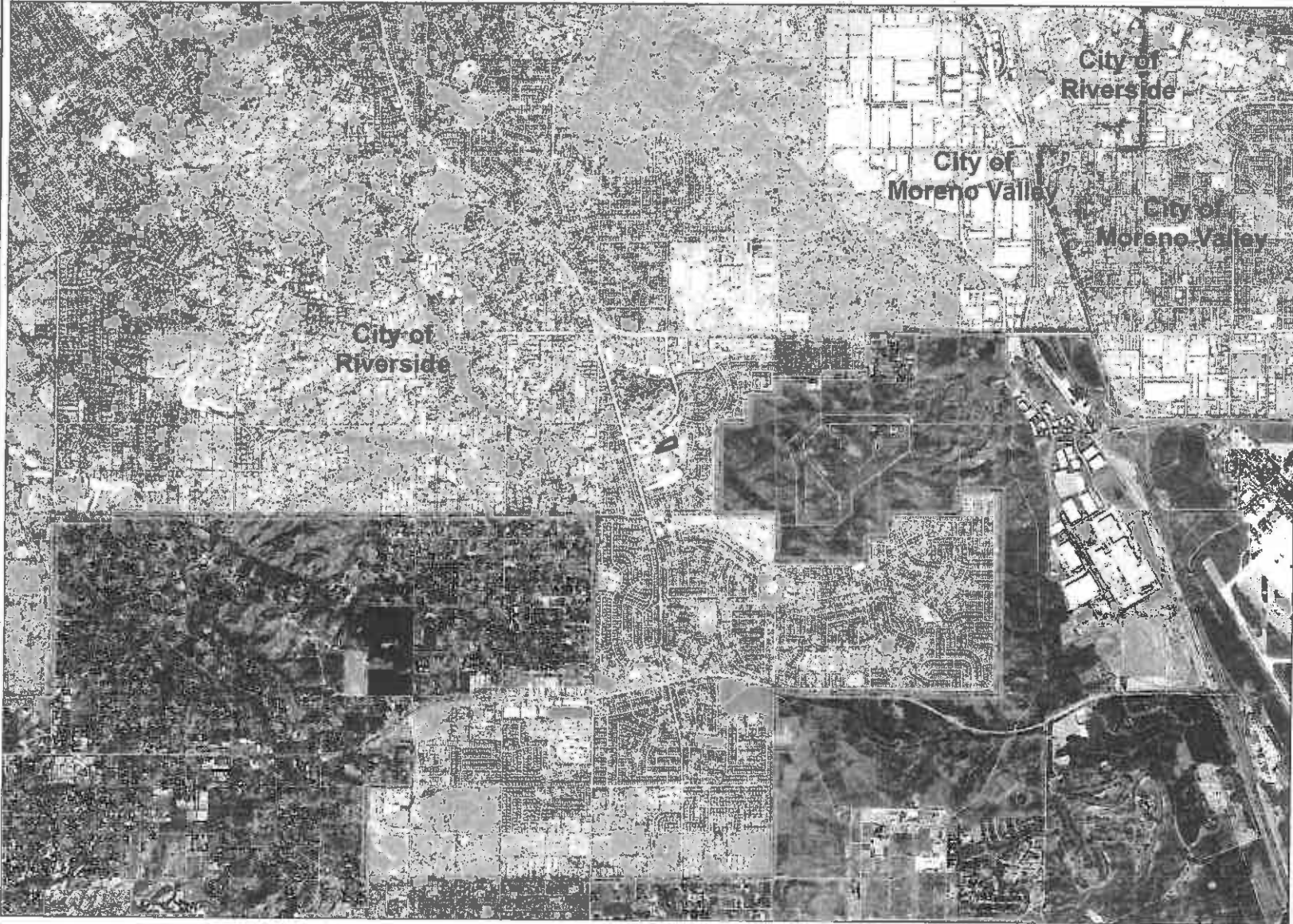
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## Notes

# My Map



## Legend

-  City Boundaries
- Cities
- highways\_large
  -  HWY
  -  INTERCHANGE
  -  INTERSTATE
  -  USHWY
-  majorroads
-  counties
-  cities
- hydrographylines
- waterbodies
  -  Lakes
  -  Rivers



0 5,423 10,846 Feet



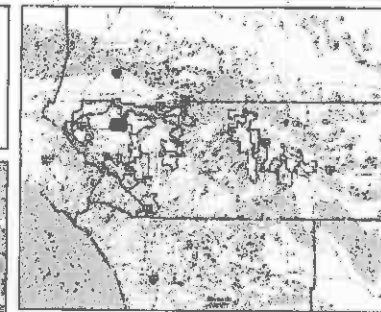
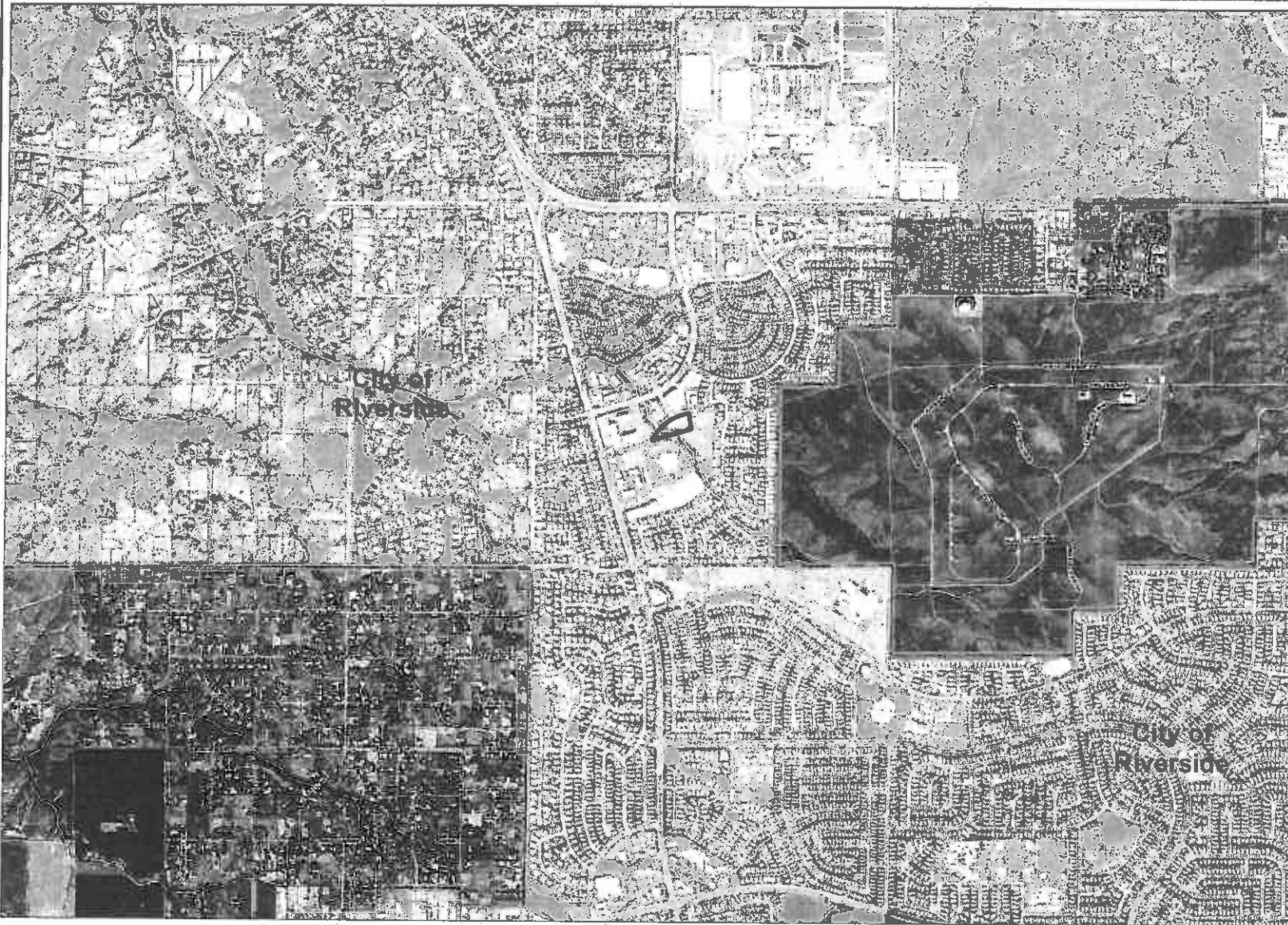
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## Notes

# My Map



## Legend

-  City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- roads
- Major Roads
- Arterial
- Collector
- Residential
-  counties
-  cities
- hydrographylines
- waterbodies
- Lakes
- Rivers



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0 2,712 5,423 Feet

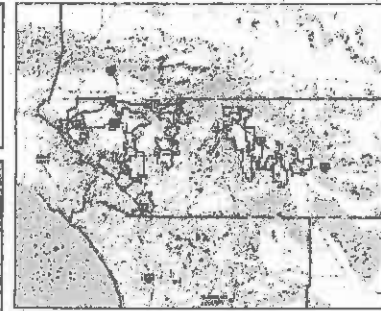


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## Notes

# My Map



## Legend

-  City Boundaries
- Cities
- roadsanno
- highways
  -  HWY
  -  INTERCHANGE
  -  INTERSTATE
  -  OFFRAMP
  -  ONRAMP
  -  USHWY
-  counties
-  cities
- hydrographylines
- waterbodies
  -  Lakes
  -  Rivers



0 678 1,356 Feet



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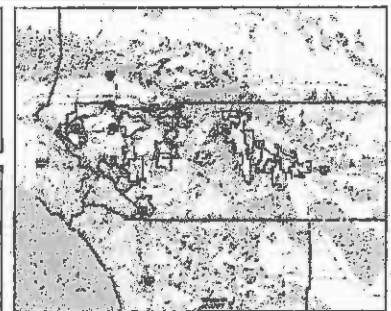
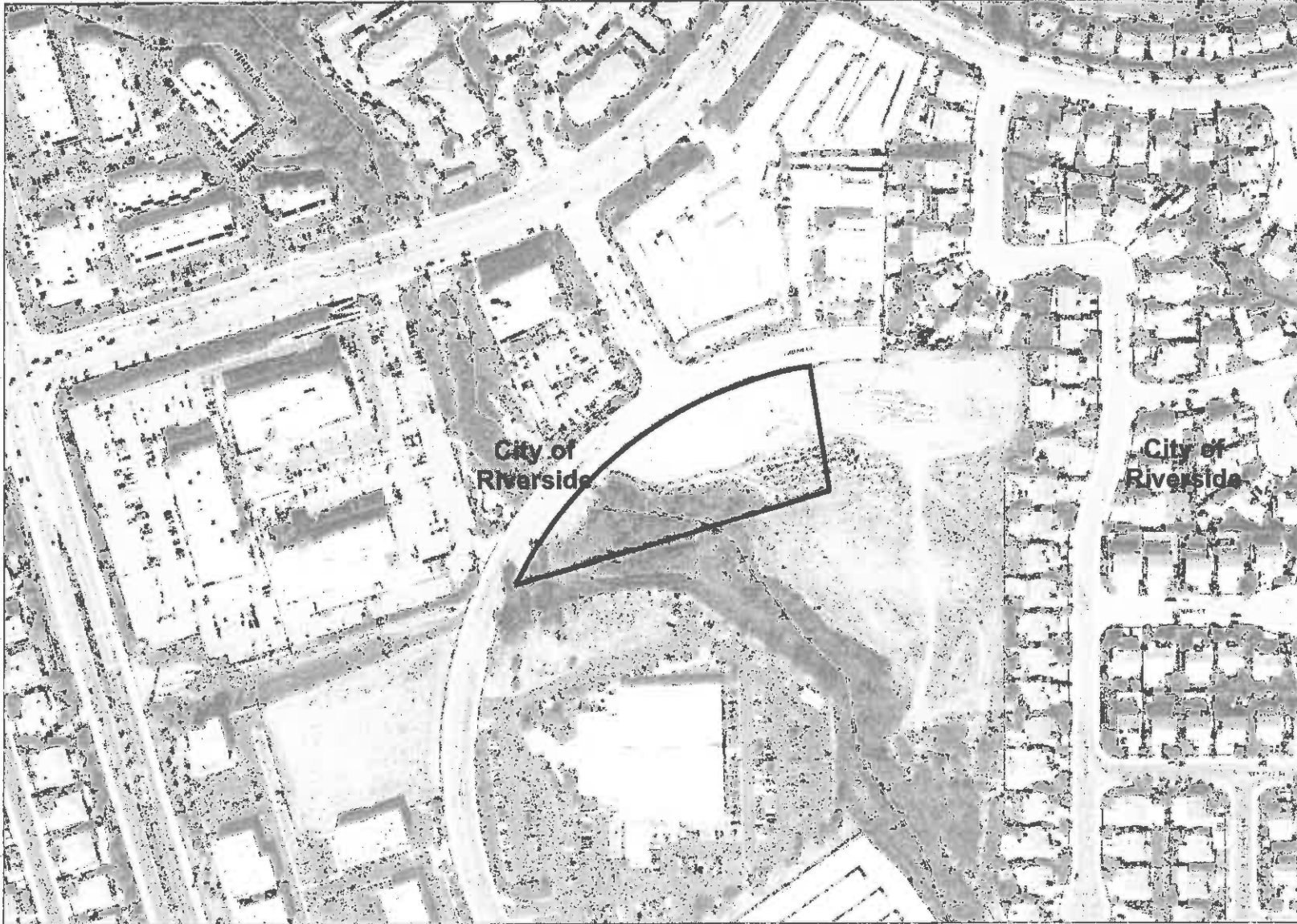
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## Notes



# My Map



## Legend

-  City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
-  counties
-  cities
- hydrographylines
- waterbodies
- Lakes
- Rivers

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## Notes

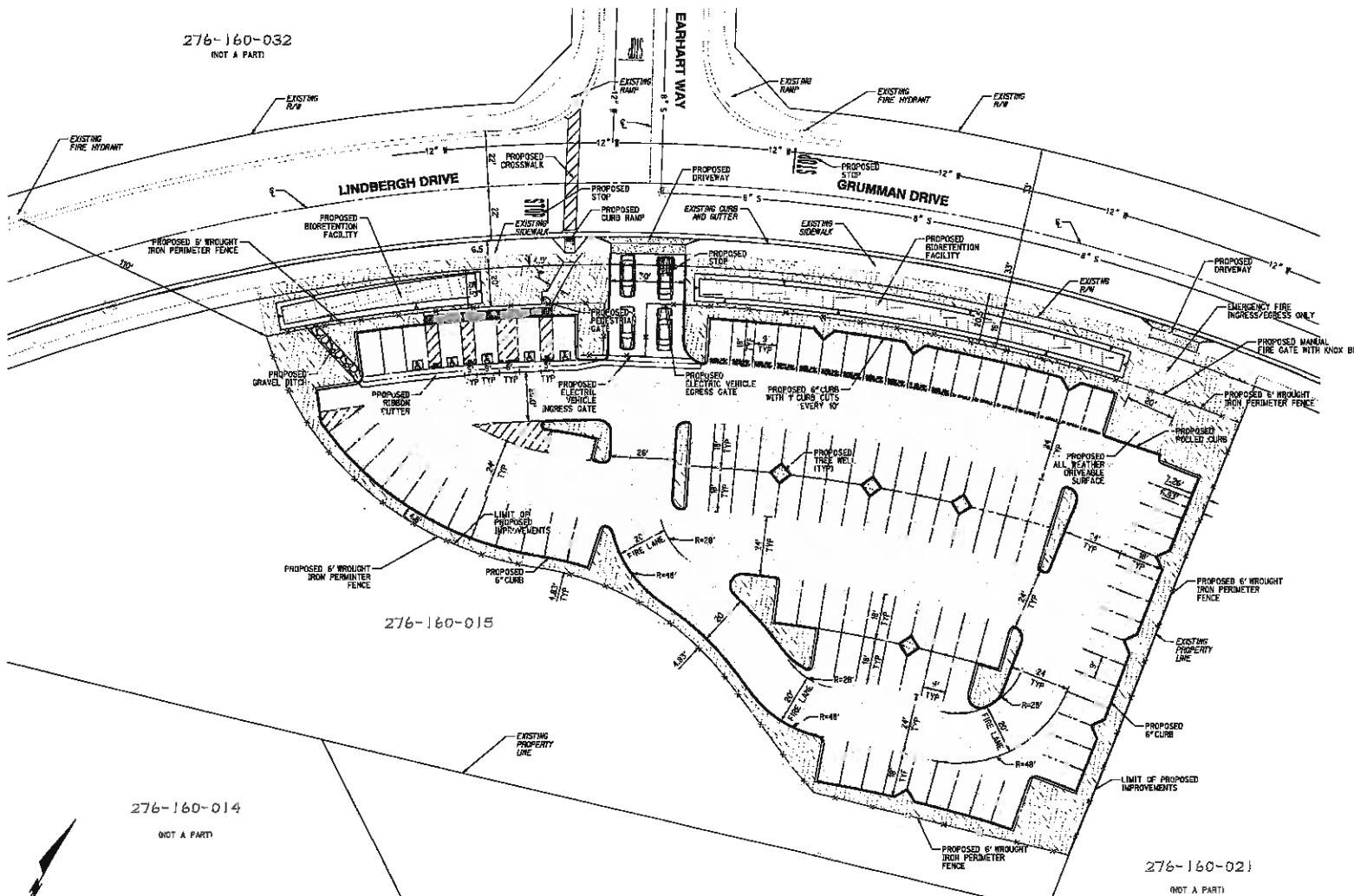


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IN THE CITY OF RIVERSIDE, CALIFORNIA  
**PLOT PLAN**  
 MISSION GROVE PARKING LOT

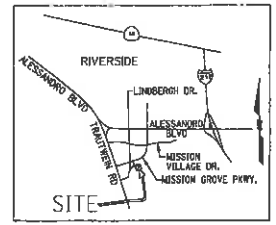
276-160-032  
 (NOT A PART)



276-160-015

276-160-014  
 (NOT A PART)

276-160-021  
 (NOT A PART)



**VICINITY MAP**

N.T.S.  
**APPLICANT/OWNER/DEVELOPER**

REGIONAL PROPERTIES  
 3041 WILSHIRE BLVD, SUITE 103  
 BEVERLY HILLS, CA 90210  
 ATTN: MARK RUBIN  
 PHONE: 310.553-1176

**ENGINEER**

RICK ENGINEERING COMPANY  
 1770 10TH AVENUE, SUITE 100  
 RIVERSIDE, CA 92507  
 PHONE: 951-782-0707  
 FAX: 951-782-0702  
 ATTN: KRISTIN WERKSMAN

**ASSESSOR'S PARCEL NUMBER**

276-160-015

PARKING COUNT	
DESCRIPTION	SPACES
STANDARD PARKING STALL	89
ADA ACCESSIBLE	5
CLEAN AIR VEHICLE	11
EV CHARGING CAPABLE	7
<b>TOTAL</b>	<b>112</b>

**TOPOGRAPHY LEGEND**

EXISTING GUARD POST	⊙
EXISTING CATCH BASIN	⊙
EXISTING SEWER CLEANOUT	⊙
EXISTING SEWER MANHOLE	⊙
EXISTING STORM DRAIN MANHOLE	⊙
EXISTING SIGN	⊙
EXISTING TREE	⊙
EXISTING UTILITY VAULT	⊙

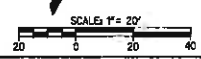
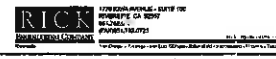
**PROPOSED LEGEND**

PLANTER AREA	▨
BIODIRECTION FACILITY	▨
CONCRETE	▨
AC PAVEMENT	▨
ALL WEATHER DRIVEABLE SURFACE	▨
TRUNCATED DOMES	▨
PERIMETER FENCE	▨

**ABBREVIATIONS**

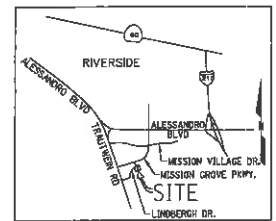
STREET CENTERLINE	⊕
TYPICAL	TYP
EXISTING	EXIST

DATE PREPARED: AUGUST 1, 2017  
 REVISED: OCTOBER 16, 2017  
**MISSION GROVE PARKING LOT**  
 CITY OF RIVERSIDE  
 IN THE COUNTY OF RIVERSIDE, CALIFORNIA



IN THE CITY OF RIVERSIDE, CALIFORNIA  
**PRELIMINARY GRADING**  
 MISSION GROVE PARKING LOT

276-160-032  
 (NOT A PART)



VICINITY MAP  
 N.T.S.

APPLICANT/OWNER/DEVELOPER

REGIONAL PROPERTIES  
 9201 WILSHIRE BLVD, SUITE 103  
 BEVERLY HILLS, CA 90210  
 ATTN: MARK RUBIN  
 PHONE: (310) 515-1776

ENGINEER

RICK ENGINEERING COMPANY  
 1770 IRWIN AVENUE, SUITE 100  
 RIVERSIDE, CA 92507  
 ATTN: KRISTIN WESCHMAN  
 PHONE: (951) 782-0707  
 FAX: (951) 782-0723

ASSESSOR'S PARCEL NUMBER

276-160-015

TOPOGRAPHY LEGEND

- EXISTING GRAB POST
- EXISTING CATCH BASIN
- EXISTING SEWER CLEANOUT
- EXISTING SEWER MANHOLE
- EXISTING STORM DRAIN MANHOLE
- EXISTING SIGN
- EXISTING UTILITY VAULT

PROPOSED LEGEND

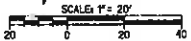
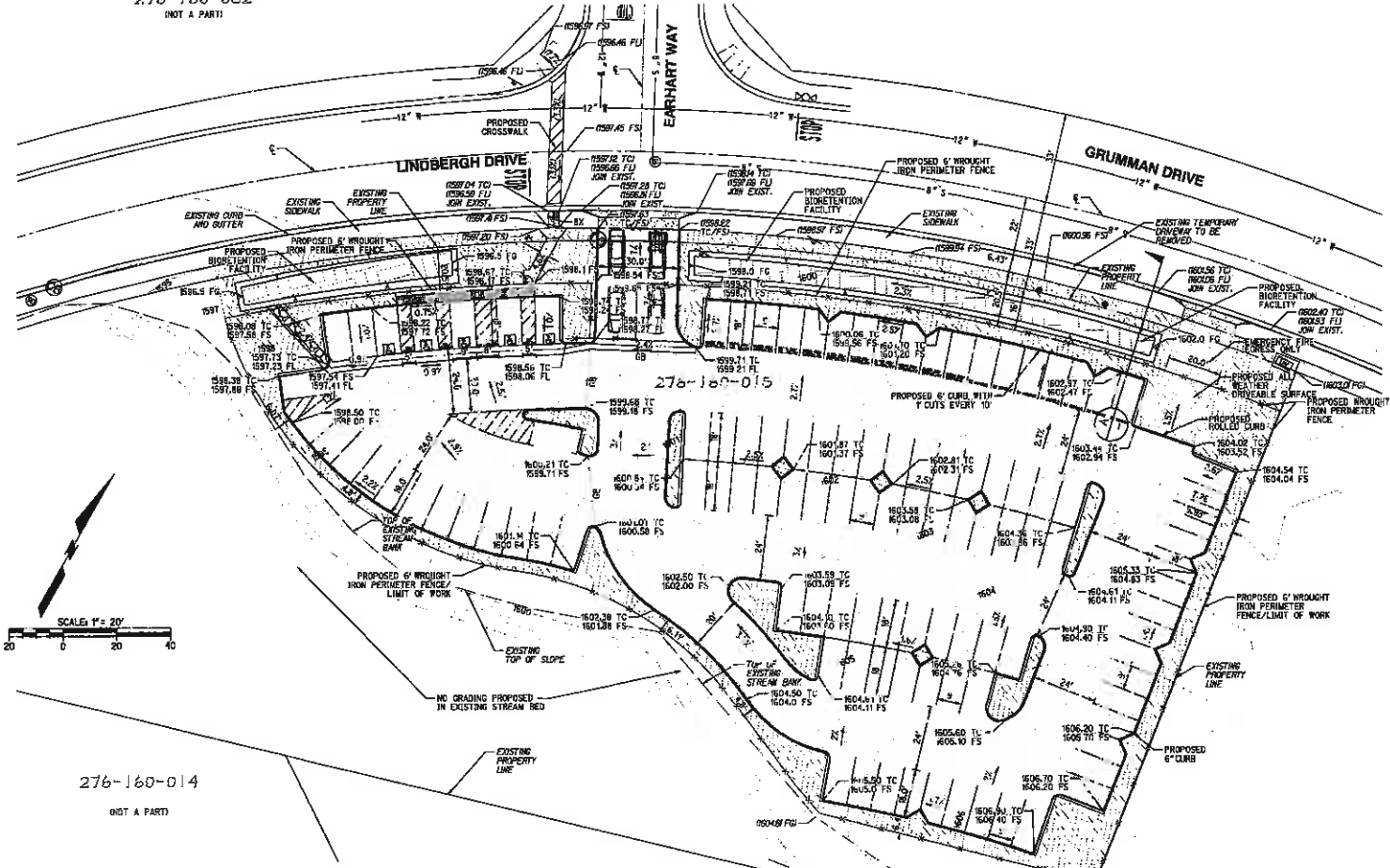
- PLANTER AREA
- BIORETENTION FACILITY
- CONCRETE
- AC PAVEMENT
- ALL WEATHER DRIVEABLE SURFACE
- TRUNCATED DOWNS
- PERIMETER FENCE

ABBREVIATIONS

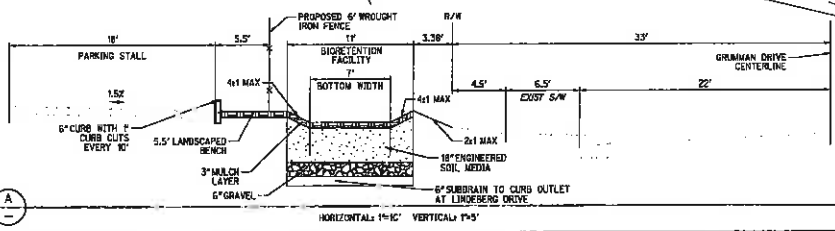
- EXISTING
- FLOW LINE ELEVATION
- FINISHED SURFACE ELEVATION
- GRADE BREAK
- RIGHT-OF-WAY
- TOP OF CURB ELEVATION

DATE PREPARED: AUGUST 1, 2017  
 REVISION: OCTOBER 16, 2017

**MISSION GROVE OFFICE PARKING LOT**  
 CITY OF RIVERSIDE  
 IN THE COUNTY OF RIVERSIDE, CALIFORNIA



276-160-014  
 (NOT A PART)



276-160-021  
 (NOT A PART)

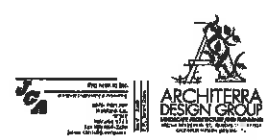
- PLAN NOTES:**
- ① 1" = 2' (1" = 1' 0" E.L.S.)
  - ② 2" = 4' (1" = 2' 0" E.L.S.)
  - ③ 3" = 6' (1" = 3' 0" E.L.S.)
  - ④ 4" = 8' (1" = 4' 0" E.L.S.)
  - ⑤ 5" = 10' (1" = 5' 0" E.L.S.)
  - ⑥ 6" = 12' (1" = 6' 0" E.L.S.)
  - ⑦ 7" = 14' (1" = 7' 0" E.L.S.)
  - ⑧ 8" = 16' (1" = 8' 0" E.L.S.)
  - ⑨ 9" = 18' (1" = 9' 0" E.L.S.)
  - ⑩ 10" = 20' (1" = 10' 0" E.L.S.)

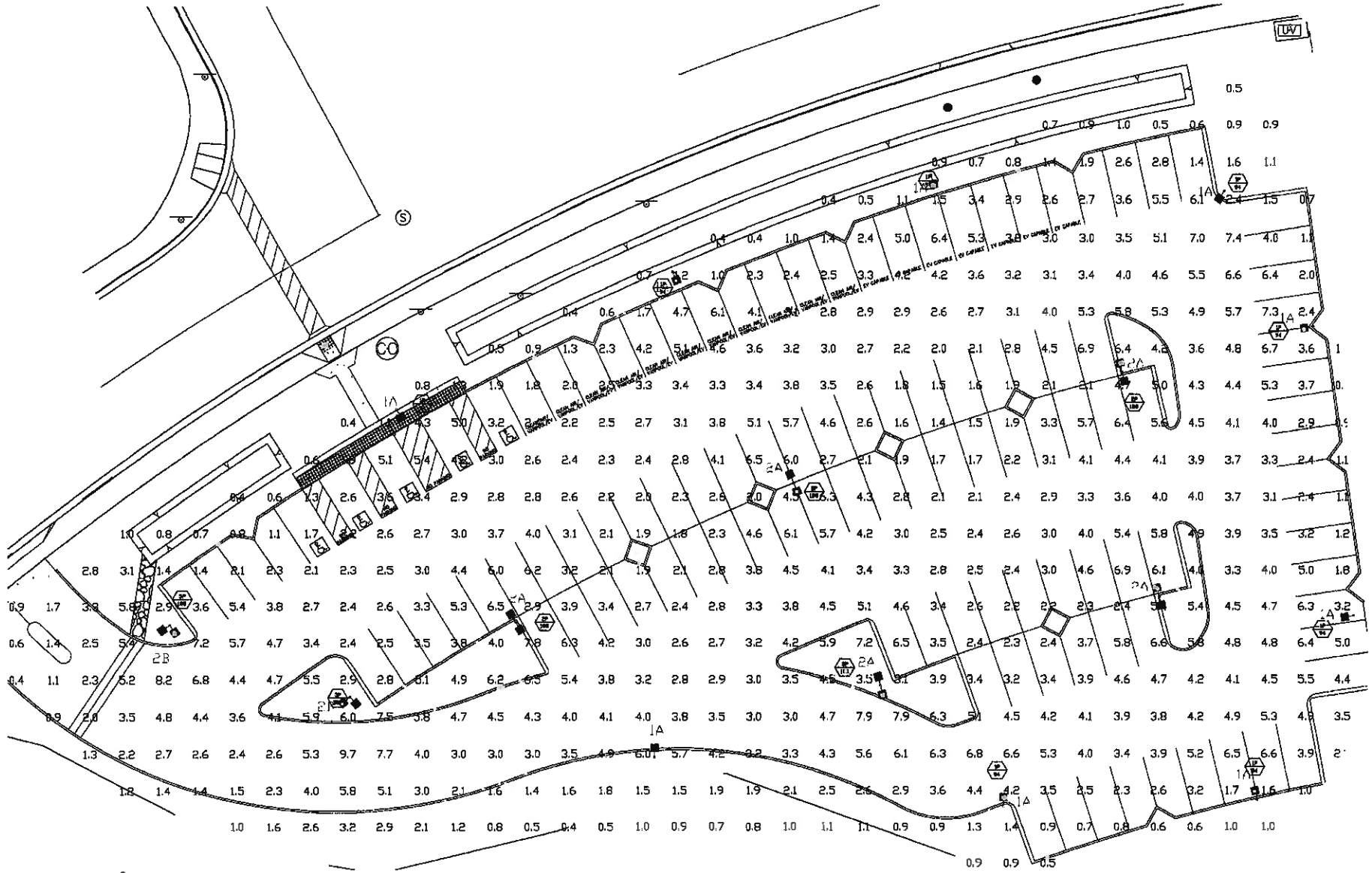


**REGIONAL PROPERTIES INC.**  
**MISSION GROVE PARKING LOT EXPANSION**  
**LANDSCAPE MASTER PLAN - ELECTRICAL LIGHTING LAYOUT**  
**MISSION GROVE, CITY OF RIVERSIDE, CA**



SCALE: 1" = 10'  
 DATE: AUGUST, 2017  
 A03 JOB # 1761  
**LC-E1**

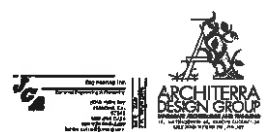




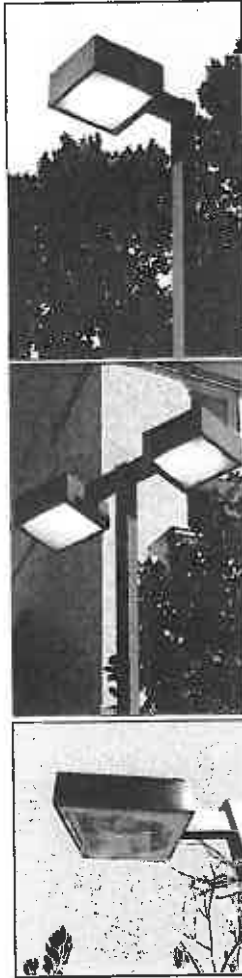
**REGIONAL PROPERTIES INC.**  
**MISSION GROVE PARKING LOT EXPANSION**  
**LANDSCAPE MASTER PLAN - PHOTOMETRIC ILLUMINATION PLAN**  
**MISSION GROVE, CITY OF RIVERSIDE, CA**



SCALE: 1" = 10'  
 DATE: AUGUST, 2017  
 AUG. 15, 2014  
**LC-E2**







**SB**

**SERIES**

The SB series is a functional modern-style fixture, cut-off with convex lens or full cutoff with flat lens for dark sky compliance.

**ARCHITECTURAL**

The housing is fabricated of .080" minimum wall thickness die-formed welded aluminum. The arm is .125" wall thickness extruded aluminum and shall attach to the fixture with two thru bolts.

The lamp is mounted, properly positioned to provide the appropriate IES light pattern.

The reflector is constructed of segmented specular aluminum available on Type 1, 2, 3, FT and 5 light distributions.

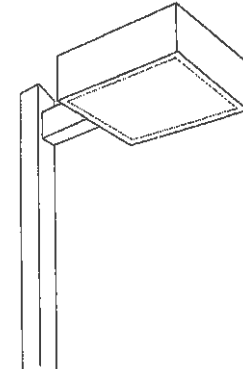
All electrical components are UL listed with electromagnetic high power factor ballast for HID lamps in voltages of 120 to 480 or an electronic ballast for compact fluorescent lamp in 120 or 277 volts. Ballasts are mounted on a removable tray for easy maintenance

*Pacific Lighting & Standards Co.*

UL LISTED FOR WET LOCATIONS  
www.pacificlighting.com

**SB**  
**SERIES**

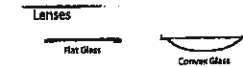
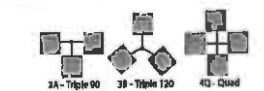
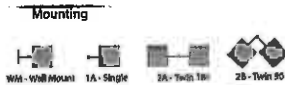
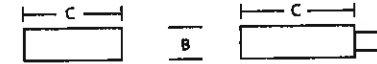
- Housing:** Heavy gauge die formed welded aluminum internally sealed for weather proofing. Closed cell neoprene foam gasket is applied between lens frame and housing.
- Lens:** Flat tempered glass or convex tempered glass sealed into cast aluminum frame.
- Electrical:** All electrical components are UL approved.
- Ballast:** High power factor - 20°C starting temperature. The ballast is removable as a unit for easy maintenance.
- Reflector:** Segmented specular Alzak, available on type 1, 2, 3, FT and 5 light distribution.
- Arms:** Extruded or/and cast aluminum.
- Finish:** Polyester powder fuse coating is standard; other finishes are available upon request.



**Ordering Information:**

1. Model Number
2. Wattage
3. Lamp Type
  - H - Metal Halide
  - S - High Pressure Sodium
  - PS - Pulse Start
4. Voltage
  - 120, 208, 240, 277, 480 and
  - MT (Multi Tap) - 120, 208, 240, 277
5. Light Distribution
  - 1, 2, 3, FT and 5
6. Lens
  - FG - Flat Glass
  - CG - Convex Glass
7. Mounting
8. Finish
  - BK - Black
  - BZ - Bronze
  - GR - Green
  - VG - Verde Green
  - (Other color upon request)
9. Options
  - RPC - Receptacle and Photo Cell
  - HS - House Shield
  - FS - Fuse
  - PC - Photo Cell

MOD#	Wattage	A	B	C	EPA
SBA	175 MAX	16"	6"	16"	.97
SBB	400 MAX	19"	6"	19"	1.1
SBC	1000 MAX	26"	10"	26"	2.1



1. Mod. #	2. Wattage 3. Lamp Type	4. Voltage	5. Light Dist.	6. Lens	7. Mounting	8. Finish	9. Options

UL LISTED FOR WET LOCATIONS

www.pacificlighting.com



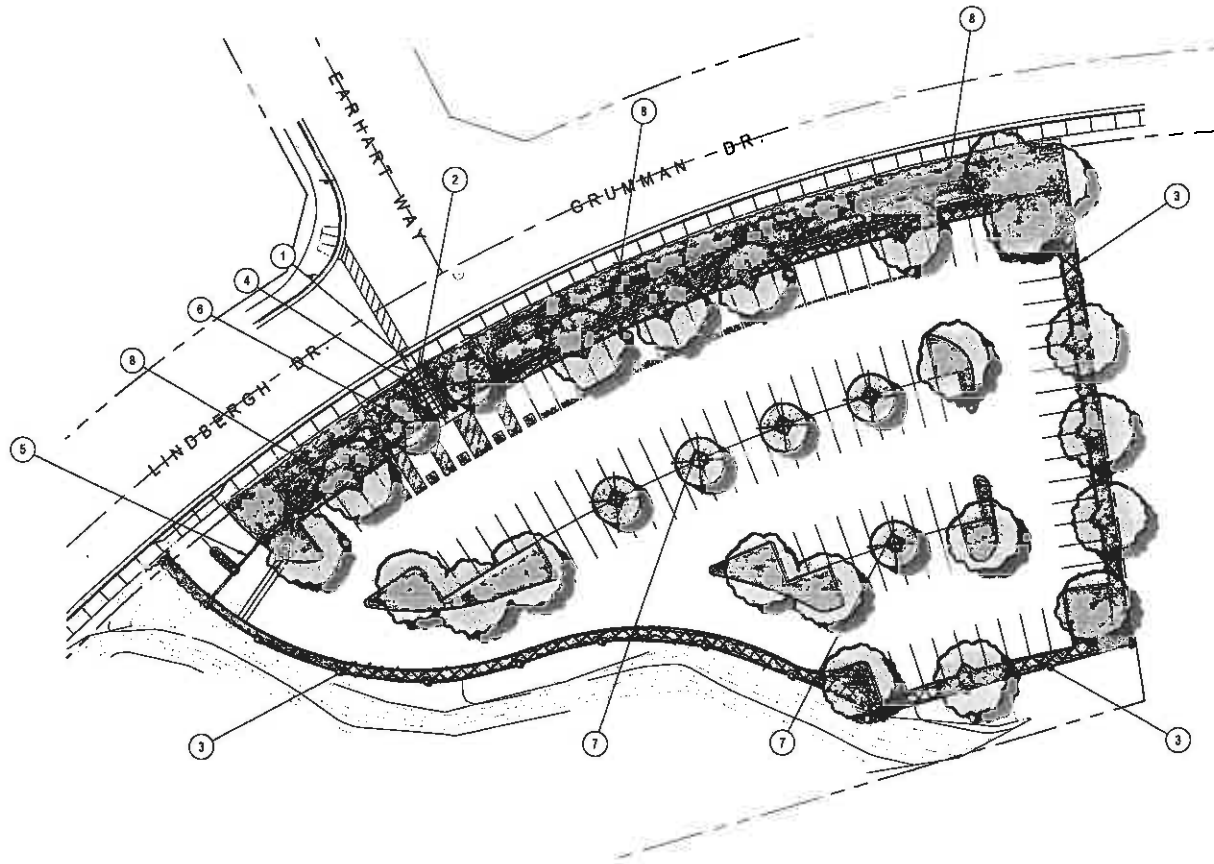
**REGIONAL PROPERTIES INC.**  
MISSION GROVE PARKING LOT EXPANSION  
LANDSCAPE MASTER PLAN - CUT SHEETS  
MISSION GROVE, CITY OF RIVERSIDE, CA



SCALE: N.T.S.  
DATE: AUGUST, 2017  
ADD JOB #: 1144

**LC-E4**





**PLANT PALETTE:**

TREES	PARKING LOT SHADE TREE	BOTANICAL NAME	COMMON NAME	QTY
	<i>Platanus x acerifolia</i>	London Plane Tree	22	
	ACCENT TREE	<i>Chilopsis linearis</i>	Desert Willow	7
DROUGHT TOLERANT SHRUBS AND GROUNDCOVER				
	<i>Callisolemon 'Little John'</i>	Dwarf Bottle Brush		
	<i>Hesperaloe parviflora</i>	Red Yucca		
	<i>Rhephilotropis indica 'Ballerina'</i>	Indian Hawthorne		
BIO-SWALE PLANTING				
	<i>Carex pansa</i>	Dune Sedge		
	<i>Leymus condensatus 'Canyon Prince'</i>	'Canyon Prince' Rye Grass		
	<i>Muhlenbergia rigida 'Washville'</i>	Purple Muhly Grass		

**FEATURE LEGEND:**

- ① Detectable Warning Tactile Warning Strips.
- ② Enhanced Paving at Pedestrian Gate to Match Adjacent Parking Lot.
- ③ Wrought Iron Fence per Others.
- ④ Wrought Iron Pedestrian Gate per Others.
- ⑤ Wrought Iron Vehicular Access Entry Gate per Others.
- ⑥ ADA Compliant Parking Stall Signage.
- ⑦ Proposed 6'-0" SQ, Diamond Tree Planters with Crushed Gravel.
- ⑧ Bio-swale with Decorative Boulders and Cobble.

**PARKING LOT CALCULATIONS:**

REGULAR PARKING STALLS	= 102
ADA COMPLIANT PARKING STALLS	= 5
ELECTRICAL VEHICLE PARKING STALLS	= 7
<b>TOTAL NUMBER OF PARKING STALLS</b>	<b>= 114</b>

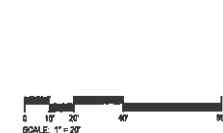
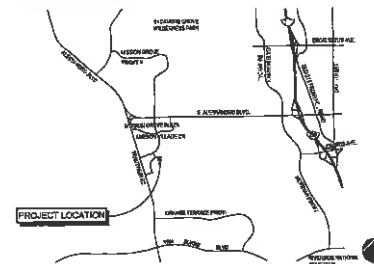
**TOTAL TREES REQUIRED:  
(1 TREE FOR EVERY 4 PARKING STALLS)**

TOTAL TREES REQUIRED	= 28.5
TOTAL PROPOSED TREES	= 29

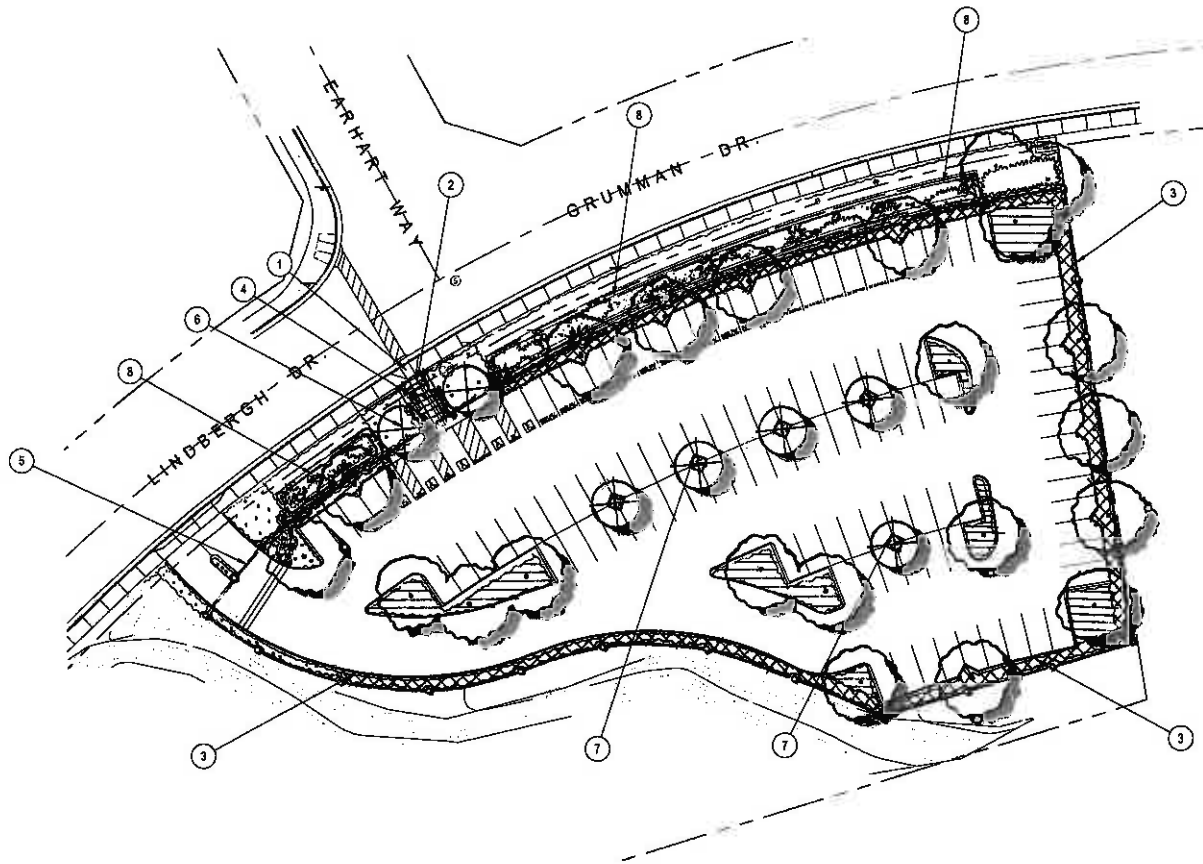


**REGIONAL PROPERTIES INC.**  
MISSION GROVE PARKING LOT EXPANSION  
LANDSCAPE MASTER PLAN - COLOR RENDER  
MISSION GROVE, CITY OF RIVERSIDE, CA

**VICINITY MAP:**







**PLANT PALETTE:**

TREES	PARKING LOT SHADE TREE	BOTANICAL NAME	COMMON NAME	QTY
		<i>Platanus x acerifolia</i>	London Plane Tree	22
		<i>Chilopsis linearis</i>	Desert Willow	7
<b>DROUGHT TOLERANT SHRUBS AND GROUNDCOVER</b>				
		<i>Callistemon 'Little John'</i>	Dwarf Bottle Brush	
		<i>Hesperaloe parviflora</i>	Red Yucca	
		<i>Rhaphirolepis indica 'Ballerina'</i>	Indian Hawthorne	
<b>BIO-SWALE PLANTING</b>				
		<i>Carex pansa</i>	Dune Sedge	
		<i>Leymus condensatus 'Canyon Prince'</i>	'Canyon Prince' Rye Grass	
		<i>Muhlenbergia rigida 'Washville'</i>	Purple Muhly Grass	

**FEATURE LEGEND:**

- ① Detectable Warning Tactile Warning Strips.
- ② Enhanced Paving at Pedestrian Gate to Match Adjacent Parking Lot.
- ③ Wrought Iron Fence per Others.
- ④ Wrought Iron Pedestrian Gate per Others.
- ⑤ Wrought Iron Vehicular Access Entry Gate per Others.
- ⑥ ADA Compliant Parking Stall Signage.
- ⑦ Proposed 6'-0" SQ. Diamond Tree Planters with crushed gravel.
- ⑧ Bio-swale with Decorative Boulders and Cobble.

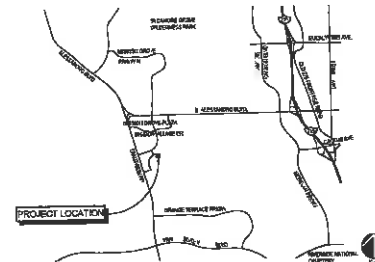
**PARKING LOT CALCULATIONS:**

REGULAR PARKING STALLS	= 102
ADA COMPLIANT PARKING STALLS	= 5
ELECTRICAL VEHICLE PARKING STALLS	= 7
<b>TOTAL NUMBER OF PARKING STALLS</b>	<b>= 114</b>

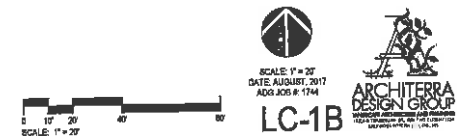
**TOTAL TREES REQUIRED:  
(1 TREE FOR EVERY 4 PARKING STALLS)**

TOTAL TREES REQUIRED	= 28.5
TOTAL PROPOSED TREES	= 29

**VICINITY MAP:**



**REGIONAL PROPERTIES INC.**  
**MISSION GROVE PARKING LOT EXPANSION**  
**LANDSCAPE MASTER PLAN - BLACK AND WHITE**  
 MISSION GROVE, CITY OF RIVERSIDE, CA



PAGE BREAK





# AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

November 9, 2017

Mr. Nathan Perez, Project Planner  
City of Perris Planning Department  
135 North D Street  
Perris CA 92570

**CHAIR**  
Rod Ballance  
Riverside

**VICE CHAIRMAN**  
Steve Manos  
Lake Elsinore

**COMMISSIONERS**

Arthur Butler  
Riverside

John Lyon  
Riverside

Glen Holmes  
Hemet

Russell Betts  
Desert Hot Springs

Steven Stewart  
Palm Springs

**STAFF**

**Director**  
Simon A. Housman

John Guerin  
Paul Rull  
Barbara Santos

County Administrative Center  
4080 Lemon St., 14<sup>th</sup> Floor.  
Riverside, CA 92501  
(951) 955-5132

[www.rcaluc.org](http://www.rcaluc.org)

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –  
DIRECTOR’S DETERMINATION**

File No.: ZAP1025RG17  
Related File No.: OA 17-05214 (Ordinance Amendment)  
APN: Citywide

Dear Mr. Perez:

As authorized by the Riverside County Airport Land Use Commission (ALUC) pursuant to its Resolution No. 2011-02, as ALUC Director, I have reviewed City of Perris Case No. 17-05214 (Ordinance Amendment), a proposal to amend Chapter 19.69 of the City’s Zoning Code (a portion of the Perris Municipal Code), which addresses Parking and Loading Standards. Specifically, the proposed amendment updates required parking ratios for multi-family housing (apartments and attached residential units), raising the number of required parking spaces from 1.0 to 1.5 spaces per unit for studio and one-bedroom units and from 1.5 to 2.0 spaces per unit for two-bedroom units. 2.5 spaces per unit would be required for units with three or more bedrooms. Additionally, one guest parking space would be required per five dwelling units.

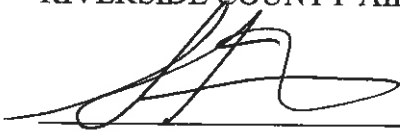
For airport land use compatibility purposes, the density of residential development is evaluated on the basis of the numbers of dwelling units per acre, not on the basis of the number of parking spaces. The number of parking spaces is one method of evaluating the density of development. As this ordinance amendment does not in itself increase density of residential uses or introduce new uses within any zoning classification, this amendment has no impact on the safety of air navigation within airport influence areas located within the City of Perris.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2004 Riverside County Airport Land Use Compatibility Plan, the 2011 Perris Valley Airport Land Use Compatibility Plan, and the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan.

If you have any questions, please contact Paul Rull, ALUC Urban Regional Planner IV, at (951) 955-6893 or John Guerin, ALUC Principal Planner, at (951) 955-0982.

## AIRPORT LAND USE COMMISSION

Sincerely,  
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



---

Simon A. Housman, ALUC Director

Attachments: Proposed Ordinance Amendment (redline copy)

cc: Gary Gosliga, Airport Manager, March Inland Port Airport Authority  
Daniel Rockholt or Denise Hauser, March Air Reserve Base  
Pat Conatser, Airport Manager, Perris Valley Airport

Y:\AIRPORT CASE FILES\Regional\ZAP1025RG17\ZAP1025RG17.LTR.doc

## CHAPTER 19.69 (DRAFT)

### PARKING AND LOADING STANDARDS

Section:

- 19.69.010 PURPOSE
- 19.69.020 RESIDENTIAL REGULATIONS
- 19.69.030 NON-RESIDENTIAL REGULATIONS
- 19.69.040 LOADING REQUIREMENTS

19.69.010 PURPOSE

Regulations shall be established for parking and loading, in order to assure adequate parking facilities are properly designed and located in order to meet the parking needs created by specific uses, and ensure their usefulness, protect the public safety, and where appropriate, buffer and transition surrounding land uses from their impact.

19.69.20 RESIDENTIAL REGULATIONS

**A. *General Provisions***

1. Amount of Facilities Required. Any dwelling unit constructed or located after the effective date of the Chapter, or any subsequent amendment thereto, shall be required to provide off-street parking facilities in accordance with the provisions of this Chapter.
2. Non-Conforming Uses. Any dwelling unit or group of dwelling units which, on the effective date of this Chapter, or any subsequent amendment thereto, is nonconforming as to the regulations relating to off-street parking facilities, may be continued in the same manner as if the parking facilities were conforming. However, any existing dwelling unit that is enlarged by 25 percent of the gross living area shall be required to provide off-street parking facilities in accordance with the provisions of this Chapter.
3. Voluntary Establishment. Nothing in this Chapter shall be deemed to prevent the voluntary establishment of off-street parking facilities in excess of those required by this Chapter, provided that all regulations governing the location, design, and operation of such facilities are met.
4. Provision is a Continuing Obligation. The required off-street parking shall be a continuing obligation. It is unlawful to discontinue or dispense with the required vehicle parking facilities without providing other vehicle-parking facilities which meet the requirements of this Chapter.

5. Relocation of Facilities. Whenever existing parking facilities are removed or converted to a permissible non-parking use, the following regulations shall apply:
  - a. Any driveway approach that no longer provides access to a covered parking facility shall be removed and replaced with standard curb and gutter, in accordance with City standards.
  - b. All paved surfaces in the front yard area that no longer provide access to a covered parking facility shall be removed and the areas landscaped.
6. Access.
  - a. Access to a parking facility shall be paved, unless said facility is located greater than 100 feet from public right-of-way. If more than 100 feet from a public right-of-way, access shall be on an all-weather surface acceptable to the City Engineer.
  - b. Driveways shall utilize concrete material, unless said driveway is greater than 45 feet in length. If more than 45 feet in length, asphaltic material may be used, subject to approval by the City Engineer.

**B. General Regulations**

1. Number of Spaces Required.
  - a. Single Family
    - 1) Light Agricultural Zone: 2 spaces, one within a garage.
    - 2) Rural Residential/Agricultural Zone: 2 spaces, one within a garage.
    - 3) Detached Residential, R4 Zone: 2 garage spaces.
    - 4) Detached Residential, R7 Zone: 2 garage spaces.
  - b. Multi-Family
    - 1) Attached Residential, R7, R14, R22 Zones: 2 spaces per unit, one within a garage; 1 guest parking space per 5 units.
    - 2) Apartments: One space per unit shall be within a carport or an enclosed garage.
      - (a) Studio Unit: ~~1 space/unit.~~ 1.5 spaces
      - (b) One Bedroom Unit: ~~1 space/unit.~~ 1.5 spaces
      - (c) Two Bedroom Unit: ~~1.5 spaces/unit.~~ 2 spaces
      - (d) Three Bedroom Unit or more: 2.5 spaces  
~~Each additional bedroom: 0.25 spaces/unit up to 10 spaces, and 0.010 spaces/unit exceeding 10 spaces. Shall be distributed throughout development.~~
      - (e) 1 guest parking space per 5 units.

2. Size.
  - a. Uncovered.

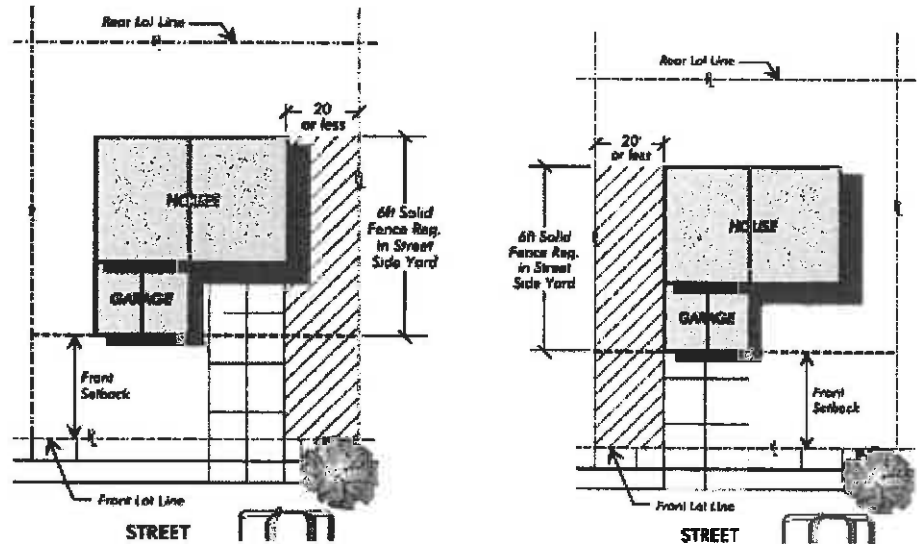
Each parking space shall have minimum dimension not less than 9 feet in width and 19 feet in length. No more than 15 percent of uncovered parking spaces for multi-family development may be compact parking stalls. Each compact parking space shall have minimum dimension not less than 8 feet in width and 16 feet in length.
  - b. Carport/Garage.

Each covered parking space in a garage or carport shall have minimum dimension not less than 10 feet in width and 20 feet in length. Minimum size for a one-car garage shall be no less than 250 square feet.
3. Location: Off-street parking facilities required by this Chapter shall be located on the same lot or parcel as the residential unit they are intended to serve.
  - a. Covered parking spaces shall not be located in any required front, side or rear yard setback.
  - b. Not more than 3 entrance bays for individual garages or carports shall face a public right-of-way.
4. Access: The following requirements shall govern access to off-street parking facilities:
  - a. Units located with sole access from an Expressway, Primary or Secondary Arterial as designated by the Perris General Plan shall provide a vehicular turn-around facility on the site to permit straight forward travel upon entering a street. Said facilities may be required for lots with sole access to a collector street.
  - b. The width of driveway entrances measured at property line shall be as follows:
    - 1) One-car facility: 10 feet minimum.
    - 2) Two-car facility: 25 feet maximum.
    - 3) Three-car facility: 35 feet maximum.
5. Recreational Vehicle Parking in Residential Zones: Parking for recreational vehicles is permissible provided the following standards are met:
  - a. Permitted Locations of Parking and Maneuvering Areas.

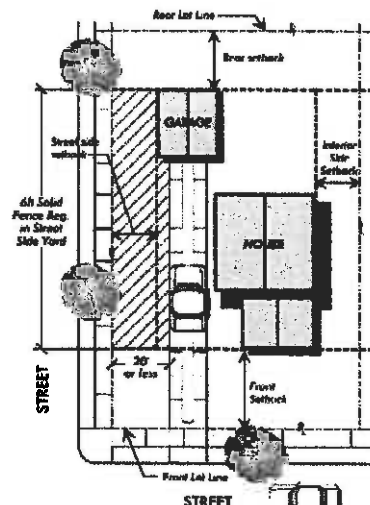
Parking and maneuvering areas of single-family residential zones for all recreational vehicles, with a gross vehicle weight rating of 10,000 pounds or less, shall be limited to the space within a carport or garage plus a paved driveway between such garage or carport and the street from which it

is served, not exceeding the width of the garage. In addition, the following front and side yard areas may also be paved for the parking and maneuvering of vehicles:

- 1) House with attached Garage: The space between the driveway serving the garage and the nearest side property line, with such paving permitted to extend as far as the rear of the residential structure, such space not to exceed twenty feet in width beyond the driveway serving the garage.

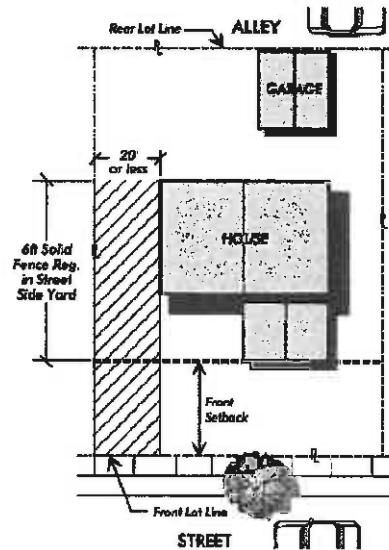


- 2) House With Detached Garage, Served by Adjacent Street: The space between the driveway and the nearest side property line, extending as far as the rear of the garage, such space not to exceed twenty feet in width beyond the driveway serving the garage.

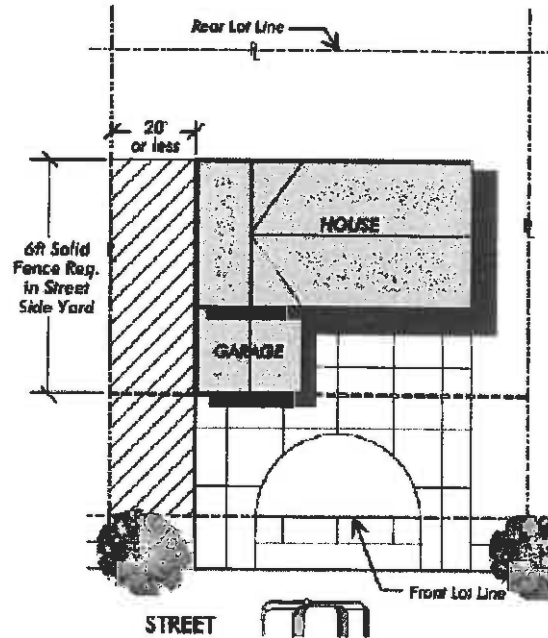




- 3) House With Detached Garage Served From an Alley: A space, not exceeding twenty feet in width, adjacent to a side property line. Such paved space may extend no further than the space between the street and the rear of the house. Installation of such a driveway is subject to approval of a driveway curb, approved by the Engineering Department.



- 4) Circular Drives: A house with one street frontage and at least one hundred feet of width, or any house with two street frontages may be served by a circular drive. In addition, the space between the circular drive and the nearest interior side property line may be paved, provided this additional paving does not exceed twenty feet in width beyond the point from the nearest point of the circular driveway and the interior side property line, nor extend further than the distance between the street and the rear of the residence. No circular drive will be approved without the approval of the Engineering Department for two driveway openings.



5) Special Requirements for Driveway Extensions in Street Side Yard Areas: Where the area proposed for driveway expansion is a street side yard, the portion of the driveway behind the front setback must be screened from adjoining street by a six-foot high solid wall to extend to the rear property line.

- b. Registration and Vehicle Condition. All recreational vehicles parked outside of a completely enclosed garage shall be currently and legally registered, except as provided for by State law and shall be in an operable and movable condition. Motorized recreational vehicles, shall be movable under their own power. Boats and other nonmotorized vehicles, such as trailers, shall be movable by a towing vehicle customarily used for the type of vehicle being towed.
- c. A recreational vehicle may not have any utility hookups, except for maintenance, or be used as living quarters.
- d. The property may be fenced subject to current Zoning Code standards.
- e. Recreation vehicles may not park or be parked or placed in such a manner that the vehicle overhangs the street, including that space between the curb line and the lot line, or in such a manner that the vehicle overhangs adjacent property, or upon any part of the sidewalk space.

6. Single Room Occupancy (SRO) unit parking shall be provided as follows (added August 2013, Ord. 1296) (see Chapter 19.28.070.E for complete development criteria):
  - a. One (1) uncovered parking space for every three (3) SRO units.
  - b. Two (2) uncovered parking spaces for an onsite manager unit.
  - c. Each SRO unit shall be provided at least one (1) lockable bicycle parking space in a location that is adjacent to that SRO unit.
  
7. Limitations and Prohibited Parking: The following limitations and prohibitions shall apply:
  - a. No parking shall be permitted in required landscaped front yard areas.
  - b. All inoperative motor vehicles shall be stored in an enclosed parking space or stored in an area screened from the street.

19.69.30 NON-RESIDENTIAL REGULATIONS

**A. *General Provisions***

1. Amount of Facilities Required: Any building or structure constructed, located, or expanded and any use of land established after the effective date of this chapter, or any subsequent amendment thereto, shall be required to provide off-street parking facilities in accordance with this Chapter.
2. Non-Conforming Uses: Any use of property which on the effective date of this Chapter, or any subsequent amendment thereto, is nonconforming as to the regulations relating to off-street parking facilities, may be continued in the same manner as if the parking facilities were conforming. However, if such parking facilities do exist they shall not be reduced.

3. Voluntary Establishment: Nothing in this Chapter shall be deemed to prevent the voluntary establishment of off-street parking facilities in excess of those required by this Chapter, provided that all regulations governing the location, design, and operation of such facilities are met.
4. Provision is a Continuing Obligation: The required off-street parking shall be a continuing obligation. It is unlawful to discontinue or dispense with the required vehicle parking facilities without providing other vehicle-parking facilities which meet the requirements of this Chapter.
5. Development Plan Approval: Development plan review for all new construction, expansion or change in use shall be submitted to and approved in accordance with Chapter 19.50 or any other applicable review procedure. This review shall include parking review and analysis.
6. Permit Requirements
  - a. No building shall be occupied and no final inspection shall be given, until off-street parking facilities are provided in accordance with the provisions of this Chapter.
  - b. No parking area shall be re-surfaced and/or re-striped without a parking plan submitted to and approved by the Planning and Community Development Department and the City Engineer.
7. Use Limitations for Required Areas: Required parking areas shall be used exclusively for vehicle parking in conjunction with a permitted use, and shall not be reduced or encroached upon in any manner.

***B. General Regulations***

1. Number of Spaces Required: Based upon the gross floor area size, the following number of spaces shall be required:
  - a. **Commercial/Office/Service Uses:**
    - 1) Neighborhood or community shopping center: 1 space for every 200 square feet.
    - 2) General retail trades: 1 space for every 250 square feet.
    - 3) Retail furniture/appliance: 1 space for every 500 square feet.
    - 4) Restaurants, cafes, bars and cocktail lounges: 1 space for every 50 square feet of dining or serving area.
    - 5) Drive-in, fast food restaurants: 1 space for every 50 square feet of dining or serving area, plus 10 additional spaces. Adequate stacking to

accommodate 8 vehicles shall be provided for drive-through lanes.

- 6) Motels/Hotels: 1.1 spaces per guest room/quarters.
- 7) Rail, bus, transportation terminals: 1 space for every 3 fixed seats in waiting area of terminal, or 1 space for every 50 square feet of waiting area in the terminal, whichever is greater.
- 8) Banks, savings and loan, and financial institutions: 1 space for every 150 square feet. Where drive-through service is provided, there shall be adequate stacking to accommodate 5 vehicles.
- 9) Plant nurseries, lumberyard, building material yards, outdoor sales of merchandise: 1 space for every 500 square feet of indoor area and outdoor sales/display area to 10 spaces and 1 space for every 2,000 square feet exceeding the 10 spaces.
- 10) Automobile & machinery sales: 1 space for every 50 square feet of indoor area and, 1 space for every 2,000 square feet of outdoor sales/display area provided that area exceeding 10,000 square feet shall require 1 space for every 5,000 square feet.
- 11) Automobile lube and tunes (quick lubes): 3 spaces for every service bay, plus adequate stacking to accommodate 1 vehicle.
- 12) Automobile repair: 5 spaces for every service bay.
- 13) Automobile washing (full service automatic): 10 spaces, plus adequate stacking to accommodate 6 vehicles.
- 14) Automobile washing (self-serving): 1 space per bay, plus adequate stacking to accommodate 1 vehicle per bay.

**b. Places of Assembly:**

- 1) Stadium/Auditorium/Arena: 1 space for every 5 fixed seats, and 1 space for every 250 square feet of non-seating area.
- 2) Private clubs/lodges: 1 space for every 50 square feet of assembly area.
- 3) Churches: 1 space for every 3 fixed seats, or 1 space for every 40 square feet of assembly area for non-fixed seating. For pews, 1 seating space is equal to 18 lineal inches of pew.
- 4) Chapels/mortuaries: 1 space for every 4 fixed seats, or 1 space for every 40 square feet of assembly area for non-fixed seating. For pews, 1 seating space is equal to 18 lineal inches of pew.

**c. Professional Services:**

- 1) Medical, dental, clinics: 1 space for every 200 square feet.
- 2) Professional offices: 1 space for every 300 square feet. (Ord. 1241, 2008)
- 3) Model home real estate complex: 5 spaces minimum, plus 2 spaces for every model home unit.
- 4) Veterinary services: 6 spaces minimum, plus 1 space for every 500 square feet in excess of 1,000 square feet.
- 5) Hospitals: 1 space for every 2 beds, based upon maximum occupancy and 1 space for every employee on the largest shift.
- 6) Urgent care centers: 1 space for every 200 square feet and one space for every employee on the largest shift.

**d. Industrial Uses:**

- 1) Manufacturing: 1 space for every 500 square feet of manufacturing building area, and 1 space for every company vehicle, and the space required for additional uses on the site; or 1 space for every employee, whichever is largest.
- 2) Warehousing: 1 space per 1,000 square feet of gross floor area for the first 20,000 square feet and 1 space per 2,000 square feet for that portion over 20,000 square feet. (Ord.1087, 2001)
- 3) High-cube warehousing: 1 space per 1,000 square feet of gross floor area for the first 20,000 square feet, plus 1 space per 2,000 square feet for the second 20,000 square feet, plus 1 space per 5,000 square feet for that portion over 40,000 square feet. Parking for office area comprising less than 10% of the total gross square footage of the building shall be included in this calculation. Office area over 10% shall be calculated at the Professional Office rate. Truck and trailer parking shall be provided at the rate of 1 oversized space for every 5,000 square feet of gross floor area. Truck docks shall not be included in this calculation. (Ord. 1256, 6/2009)
- 4) Mini-warehousing: 1 space for every 2,000 square feet of warehouse area, and appropriate number of spaces for associated office area and caretaker's residence.
- 5) Salvage/junk/auto wrecking yards: 1 space for every 5 vehicle storage spaces, to 10 spaces, and 1 space

for every 12 spaces in excess of 10 spaces, based upon maximum occupancy.

- 6) Storage yards: 1 space for every 1,000 square feet, plus spaces required for additional uses on the site.

**e. Educational Services and Care Facilities:**

- 1) Child Care: 1 space for every 5 children, and 1 space for every employee on the largest shift. A minimum of 4 spaces shall be provided. A semicircular drive or its equivalent, with sufficient space for 1 passenger loading area.

2) Schools:

- i. Grades K-8: 5 spaces, plus 2 spaces per classroom. A semicircular drive or its equivalent with sufficient space for 2 passenger loading areas.
  - ii. Grades 9-12: 1 space for every 3 students enrolled and 1 space for every employee on the largest shift.
  - iii. Colleges/Universities: 1 space for every 3 enrolled, full time day students and 1 space for every employee on the largest shift.
  - iv. Trade/Vocational Business School: 1 space for every 3 students, plus 1 space for every employee on the largest shift.
- 3) Residential Care Facility/Group Homes: 1 space for every 2 residences, based upon maximum occupancy, and 1 space for every employee on the largest shift.
  - 4) Convalescent Hospital/Nursing Homes: 1 space for every 4 residences, based upon maximum occupancy, and 1 space for every employee on the largest shift.
  - 5) Emergency Shelter Facilities: 1 space per 4 beds, and/or 0.5 per bedroom designated as a family unit with children, plus 1 space per staff member. Bike rack parking for a minimum of 10 bicycles shall be provided at the facility. (added Aug 2013, Ord. 1296)

**f. Entertainment/Recreation**

- 1) Dance/Night Clubs: 1 space for every 20 square feet of dance area, and 1 space for every 3 fixed seats, or 1 space for every 20 square feet of seating area where there are no fixed seats.
- 2) Amusement Enterprises: 1 space for every 4 persons attending, based upon maximum attendance.

- 3) Golf Courses/Driving Ranges: 3 spaces for every hole, or one space per green tee, plus the spaces required for additional uses on the site.
  - 4) Miniature Golf: 6 spaces for every hole, and the spaces required for additional uses on the site.
  - 5) Billiards Parlors: 2 spaces for every table.
  - 6) Skating Rinks: 1 space for every 3 fixed seats, and 1 space for every 250 square feet.
  - 7) Pools (commercial): 1 space for every 100 square feet of water surface area and 1 space for every employee, with a 10 space minimum.
  - 8) Tennis/Handball/Racquetball: 2 spaces for each court.
  - 9) Theaters/Movie Houses: 1 space for every 4 seats, plus 5 employee spaces.
  - 10) Arcades: 1 space for every 3 machines.
  - 11) Bowling Alleys: 4 spaces for every lane, and the spaces required for additional uses on the site.
  - 12) Gyms/Health Clubs: 1 space for every 250 square feet.
  - 13) Parks: 1 space for every 8,000 square feet of active recreative area, and one space for every acre of passive area.
2. Number of Spaces for Unspecified Uses: any use not specifically listed herein, shall be determined by the Planning and Community Development Director. Such determination shall be based upon the requirements of comparable uses as specified herein.
  3. Bicycle Parking: Facilities with 200 or more required parking spaces may provide a bicycle parking area to accommodate no less than 5 locking bicycles. Facilities with 500 or more required parking spaces may provide a bicycle parking area to accommodate no less than 15 locking bicycles. Bicycle parking areas shall be located near main entrances or buildings. For every 2 bicycle spaces provided, credit for one vehicle parking space shall be given.
  4. Motorcycle Parking: Facilities with 200 or more required parking spaces may provide a motorcycle parking area with an overall dimension of 7 feet in length and area not less than 56 square feet. Facilities with 500 or more required parking spaces may provide a motorcycle parking area with an overall dimension of 7 feet in length and area not less than 70 square feet. For every 2 motorcycle spaces provided, credit for 1 vehicle parking space may be given.



5. Shared Parking Provisions: Shared parking may be approved provided that times of operation of the involved entities are not the same, as specified herein:
- a. Up to 50 percent of the parking facilities required by this Chapter for a use considered to be primarily a day time (on-peak) use may be provided by a use considered to be a night time or Sunday (off-peak) use; up to 50 percent of the parking facilities required by this Chapter for a use considered to be a night time or Sunday use may be provided by a use considered to be primarily a daytime use, provided that a reciprocal parking area shall be subject to conditions as stipulated herein.
  - b. The following uses are considered to be daytime uses: Banks, business and professional offices, retail stores, service shops and similar uses. The following uses are considered to be night time or Sunday uses: auditoriums, churches, fraternal organizations and theatres. The Development Services Department shall determine the parking requirements of the uses proposed for shared parking.
  - c. Conditions required for shared parking:
    - 1) Shared parking facility areas shall be located within 200 feet of the buildings and uses.
    - 2) Applicant shall demonstrate that there is no substantial conflict in the principal operating hours for the buildings and uses.
    - 3) Parties concerned in the shared use of off-street parking facilities shall execute an agreement for such use by a proper legal instrument approved by the City Attorney as to form and content.
6. Transportation Demand Management Ordinance: The requirements and provisions of the Transportation Demand Management Ordinance shall be complied with in accordance with Chapter 7.40, including but not limited to reduced parking provisions, parking analysis, and penalties.
7. Handicapped Spaces: Handicapped parking spaces shall be provided in accordance with the requirements of State and Federal Law. The parking standards within this Chapter are in accordance with those established by the State and Federal Government at the time of adoption of this Chapter. Any changes in the State or Federal requirements for handicapped standards shall preempt the affected requirements of this Chapter.

- a. Space Size: Each parking space designated for use by the handicapped shall consist of a rectangular area not less than 14 feet wide by 19 feet long. When more than 1 space is provided, in lieu of providing a 14 foot wide parking stall, provide a 9 foot parking on each side of a 5 foot loading and unloading area in the center.
- b. Van Accessible: Each van accessible parking space designated for use by the handicapped shall consist of a rectangular area not less than 17 feet wide by 19 feet long. When more than 1 space is provided, in lieu of providing a 17 foot wide parking stall, provide a 9 foot parking space on each side of an 8 foot loading and unloading area in the center.
- c. Labeling: All handicapped parking stalls shall be individually labels and signed in accordance with State and Federal requirements.
  - 1) Handicap parking sign 80" high
  - 2) Typical symbol/blue field 48" x 48" – white wheelchair 36" x 36".
  - 3) 48" minimum walkway with curb cut style ramp, not to exceed a 1:12 slope.
- d. Handicapped parking spaces required by this Chapter shall count toward fulfilling off-street parking requirements.
- e. Handicapped parking spaces shall be provided for all uses at the following rate:

TOTAL NUMBER OF PARKING SPACES PROVIDED	NUMBER OF HANDICAPPED STALLS
1-25	1
26-50	2
51-75	3
76-100	4
101-150	5
151-200	6
201-300	7
TOTAL NUMBER OF PARKING SPACES PROVIDED (con't.)	NUMBER OF HANDICAPPED STALLS
301-400	8
401-500	9
501-1000	2% OF TOTAL
1001 +	20 +1 FOR EACH 100 OVER 1,000

- f. Handicapped spaces shall be located to provide for safety and optimum proximity to the entrances of greatest incidence of use when more than one building is served by the parking lot. Such spaces shall be located so that a handicapped person is not compelled to wheel or walk behind parked vehicles.

**C. *Development Standards***

1. Location of Parking Facilities.

- a. Parking facilities required by this Chapter shall be located on the same lot or parcel of land as the use they are intended to service, except in cases of large centers with reciprocal access and parking agreements, and uses with approved shared parking agreements.
- b. Parking facilities shall be located and oriented to access the main entrance or front of buildings. Parking shall not be located behind buildings to the greatest extent possible.

2. Dimensions of Parking Spaces.

- a. Each off-street parking space shall have a dimension not less than 9 feet in width and 19 feet in length, except parallel parking stall which shall be a minimum of 8 feet in width and 24 feet in length. No part of the area of a required parking space shall be utilized for driveway, aisles, walkway or other required improvements.
- b. A compact parking stall shall be permitted subject to the following:
  - 1) A compact stall shall be a dimension not less than 8 feet in width and 16 feet in length.
  - 2) Compact stalls shall be permitted for projects that provide more than 40 off-street parking spaces.
  - 3) Developments with 40 to 800 required off-street parking may be permitted to utilize a maximum of 15 percent compact stalls. Developments with 800 or more required off-street parking may be permitted to utilize a maximum of 25 percent compact stalls.
  - 4) Compact spaces shall be dispersed throughout the development, and not be located at the main entrance.
  - 5) Compact spaces shall be designated "COMPACT" and shall be visible day and night.

3. Access to Off-Street Parking: The following requirements shall govern access to off-street parking facilities:

- a. Forward travel to and from parking facilities from a dedicated street or alley is required. The parking area shall

be adequate to facilitate the turning of vehicles to permit forward travel upon entering a street.

- b. All uses, which adjoin an Expressway, Primary or Secondary Arterial street, as designated in the Perris General Plan, shall whenever possible minimize the number of access points, to alleviate the proliferation of driveways.
- c. The access to all off-street parking facilities shall be designed in a manner which will not interfere with the safe movement of traffic.
- d. Entryway to parking areas shall be well-defined and recognizable with adequate lighting and signage provided to facilitate adequate movement on-site and off-site.
- e. Concrete and/or accented paving driveway approaches shall be provided for ingress to and egress from all parking facilities. Each parking space shall be easily accessible to the intended user. The width of driveway entrances and exits from a public street shall be measured at the property line and shall comply with the following standards, unless specific exceptions are made by the City Engineer:
  - 1) Minimum driveway width for single-lane entrances and/or exits: 20 feet.
  - 2) Minimum driveway width for combined entrances and exits: 26 feet.
  - 3) Maximum driveway width for multiple entrances and exits: 60 feet.

4. Circulation Within Parking Area.

- a. Minimum aisle width for two-way circulation shall be 26 feet, unless otherwise specified. In areas commonly used by oversized vehicles, such as delivery and loading areas, the minimum aisle width shall be 30 feet.
- b. Two-way circulation with perpendicular parking is encouraged; however, angled parking with one-way circulation is permissible within parking areas subject to the following regulations:

Parking Angle	Stall Width	Stall Depth	Aisle Width
0 degrees	8 feet	24 feet	15 feet
30 degrees	9 feet	19 feet	15 feet
45 degrees	9 feet	20 feet	16 feet
60 degrees	9 feet	21 feet	18 feet
90 degrees	9 feet	19 feet	24 feet

- 5. Improvements to Parking Areas: All required off-street parking areas shall have the following improvements:

- a. All off-street parking areas and vehicle sales areas, and any driveways used for access shall be paved. Acceptable means of paving shall include: Asphaltic, concrete or other permanent, impervious material as approved by the City Engineer.
- b. Individual parking stalls shall be legibly marked off on the pavement by means of painting or contrasting materials. Arrows painted on paving shall dictate direction of traffic flow. Parking stall striping, directional arrows and parking stall identification shall meet the following standards:
  - 1) All parking stalls shall be clearly striped and permanently maintained with 4 inch side double or hairpin lines on the surface of the parking facility, with the two lines located an equal distance of 9 inches on either side of the stall sidelines.
  - 2) All drive aisles, entrances and exits shall be clearly marked with directional arrows painted on the parking surface.
- c. Vehicle overhangs shall be permissible, subject to the following provisions:
  - 1) Vehicle overhang may encroach into a landscaped area provided that a minimum landscape area is not less than 7 feet in width, and the landscape overhang area is above and beyond that of the amount of landscape area required.
  - 2) Vehicle overhang may encroach into a walkway provided that a minimum unimpeded walkway width not less than 5 feet is maintained.
- d. Wheel stops shall be required where necessary to alleviate any conditions that may result in vehicular damage to on-site facilities.
- e. Parking area surfaces shall be graded and drained so as to dispose of all surface water. Drainage shall be taken to the curb or gutter and away from adjoining property. Such drainage facilities shall not be allowed to cross the surface of a public sidewalk.
- f. Walkways for pedestrians shall be provided to connect parking areas to destination points. Walkways shall be paved, lighted and have adequate marking for easy identification and direction of pedestrian traffic.
- g. Landscaping.
  - 1) Landscaping shall be dispersed throughout the parking area, at a rate of one tree for every 6 stalls. Trees within parking areas shall be a minimum size of 15 gallon, with no less than 25 percent having a minimum size of 24 inch box.

- 2) A 6 inch curb with a 12 inch concrete walkway shall be constructed along the planter on end stalls adjacent to vehicle parking areas to facilitate access to parked vehicles.
  - 3) All landscaping shall be within planters bounded by a curb at least 6 inches wide and 6 inches high.
- h.** Parking areas shall have lighting capable of providing illumination for security and safety. The minimum requirement is one foot candle, maintained across the surface of the parking area. Lighting standards shall be energy efficient and in scale with the height and use of the structure. Any illumination, including security lighting, shall utilize full-cut-off fixtures, and be directed away from adjoining properties and public right-of-way
- i.** Parking areas used for private and public parking shall be developed and maintained in good condition and in accordance with the provisions of this Chapter.

#### 19.69.40 LOADING REQUIREMENTS

##### ***A. General Regulations***

1. All non-residential uses shall provide loading spaces not less than 10 feet in width, 20 feet in length, and 14 feet in height, except for those spaces intended for use by tractor trailers which shall be a minimum of 12 feet in width 45 feet in length and 14 feet in height, at the rate as follows:
  - a. Commercial Buildings
    - 1) 10,000 – 25,000 SF of building area: 1 loading space.
    - 2) 25,001 – 50,000 SF of building area: 2 loading spaces.
    - 3) 50,001 – 75,000 SF of building area: 3 loading spaces.
    - 4) 75,001 – 105, 000 SF of building area: 4 loading spaces.
    - 5) 105,001 or more SF of building area: 5 loading spaces.
  - b. Industrial Buildings
    - 1) 20,000 – 50,000 SF of building area: 1 loading space.
    - 2) 50,001 – 100,000 SF of building area: 2 loading spaces.
    - 3) 100,001 – 150,000 SF of building area: 3 loading spaces.
    - 4) 150,001 or more SF of building area: 4 loading spaces.

c. Institutional Hospital Buildings

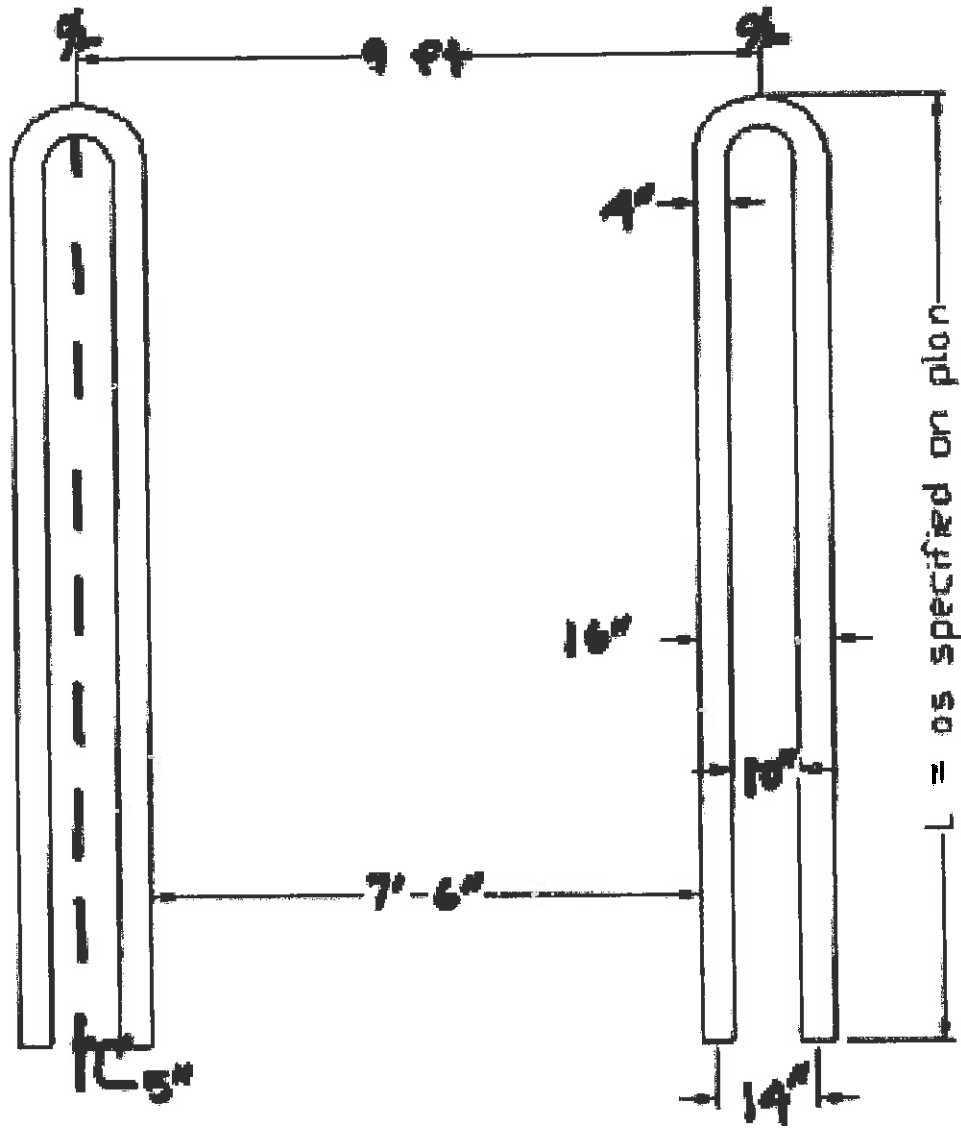
- 1) 5,000 – 20,000 SF of building area: 1 loading space.
- 2) 20,001 – 50,000 SF of building area: 2 loading spaces.
- 3) 50,001 – 75,000 SF of building area: 3 loading spaces.
- 4) 75,001 – 126,000 SF of building area: 4 loading spaces.
- 5) 125,001 or more SF of building area: 5 loading spaces.

d. Office/Service Buildings

- 1) 10,000 – 50,000 SF building area: 1 loading space.
- 2) 50,001 – 100,000 SF building area: 2 loading spaces.
- 3) 100,001 + SF building area: 3 loading spaces.

2. All loading facilities and maneuvering areas shall be located on-site.
3. Sites shall be designed so that parking areas are separate from loading areas, and loading areas are oriented to the rear or side of buildings.
4. Loading facilities shall be screened from the public view by use of walls, landscaping or building design and/or placement.
5. Loading areas shall be designed as an integral part of the building architecture and site design.

# Parking Stall Striping Detail



9' x 19'



PAGE BREAK





# AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

October 26, 2017

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Riverside

**VICE CHAIRMAN**  
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**STAFF**

**Director**  
Simon A. Housman

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County Administrative Center  
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Riverside, CA 92501  
(951) 955-5132

[www.rcaluc.org](http://www.rcaluc.org)

Ms. Lisa Gordon, Planning Manager  
City of Menifee Community Development Department  
29714 Haun Road  
Menifee CA 92586

## RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW – DIRECTOR'S DETERMINATION

File No.: ZAP1285MA17 – Letter 1 of 2  
Related File Nos.: SP 2017-187 (Specific Plan), CZ 2017-188 (Change of Zone)  
APNs: 333-020-009 and -010, 333-030-012 and -013, -021, -022

Dear Ms. Gordon:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to ALUC Resolution No.15-01 (as adopted on August 13, 2015), staff reviewed City of Menifee Case Nos. SP 2017-187 (Specific Plan), a proposal to establish a new "Fleming Ranch" Specific Plan with medium-density residential community and freeway-oriented commercial development on 331 acres located easterly of Encanto Drive and Interstate 215, southerly of Rouse Road, and westerly of Antelope Road and CZ 2017-188, a proposal to change the zoning classification of the site from One-Family Dwellings (R-1) and Scenic Highway Commercial (C-P-S) to Specific Plan (SP). The proposed Specific Plan would provide for up to 1,080 dwelling units on 222.5 acres, 20.4 acres of commercial development, 12.9 acres of open space, and 36.6 acres of roadways.

The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E, residential density and nonresidential intensity are not restricted.

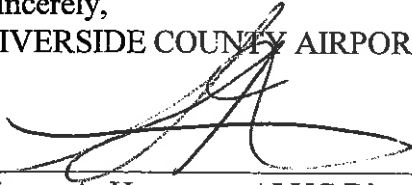
As ALUC Director, I hereby find the above-referenced Specific Plan and Zone Change **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan ("March ALUCP").

This finding of consistency relates to airport compatibility issues and does not necessarily constitute an endorsement of the proposed Specific Plan and Zone Change. As the site is located within Compatibility Zone E, both the existing and proposed zoning of this property are consistent with the March ALUCP.

If you have any questions, please contact Paul Rull, ALUC Urban Regional Planner IV, at (951) 955-6893 or John Guerin, ALUC Principal Planner, at (951) 955-0982.

## AIRPORT LAND USE COMMISSION

Sincerely,  
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



---

Simon A. Housman, ALUC Director

Attachments: Notice of Airport in Vicinity

cc: BLC Fleming LLC, Noah Shih (applicant/property owner)  
K&A Engineering, Inc. (representative)  
The Fleming Family Limited Partnership (property owner)  
Gary Gosliga, Airport Manager, March Inland Port Airport Authority  
Denise Hauser or Daniel Rockholt, March Air Reserve Base  
ALUC Case File

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# AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY



October 26, 2017

Ms. Lisa Gordon, Planning Manager  
City of Menifee Community Development Department  
29714 Haun Road  
Menifee CA 92586

**CHAIR**  
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Riverside

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**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –  
DIRECTOR’S DETERMINATION**

File No.: ZAP1285MA17 – Letter 2 of 2  
Related File No.: 2017-264 (Tentative Tract Map No. 37391)  
APNs: 333-020-009 and -010, 333-030-012 and -013, -021, -022

Dear Mr. Gordon:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed City of Menifee Case No. 2017-264 (Tentative Tract Map No. 37391), a proposal to divide 331 acres located easterly of Encanto Drive and Interstate 215, southerly of Rouse Road, and westerly of Antelope Road, into 17 lots largely corresponding to individual residential Planning Areas of the proposed Fleming Ranch Specific Plan.

**STAFF**

**Director**  
Simon A. Housman

John Guerin  
Paul Rull  
Barbara Santos

County Administrative Center  
4080 Lemon St., 14th Floor.  
Riverside, CA 92501  
(951) 955-5132

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The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, residential density is not restricted.

Although the project is located within the March Air Reserve Base/Inland Port AIA, the actual nearest runway is Runway 15-33 at Perris Valley Airport. The southerly terminus of this runway is located approximately 14,000 feet from the project site. At this distance, given the runway elevation of 1,415 feet above mean sea level (AMSL), Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,555 feet AMSL. The site has an existing maximum elevation of approximately 1,430 feet above mean sea level. With a maximum structure height of 45 feet, the top point elevation would be 1,475 feet AMSL. Therefore, Federal Aviation Administration (FAA) obstruction evaluation review for height/elevation reasons is not required.

As ALUC Director, I hereby find the above-referenced Tentative Tract Map **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, provided that the City of Menifee applies the following recommended conditions:

**CONDITIONS:**

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be

## AIRPORT LAND USE COMMISSION

downward facing.

2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris centers, fly ash disposal, and incinerators.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The attached notice shall be provided to all potential purchasers of the proposed lots and to tenants of the homes thereon.
4. All new aboveground detention or bioretention basins on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention/bioretention basin(s) that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.

If you have any questions, please contact Paul Rull, ALUC Urban Regional Planner IV, at (951) 955-6893 or John Guerin, ALUC Principal Planner, at (951) 955-0982.

Sincerely,  
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

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Simon A. Housman, ALUC Director

Attachments: Notice of Airport in Vicinity

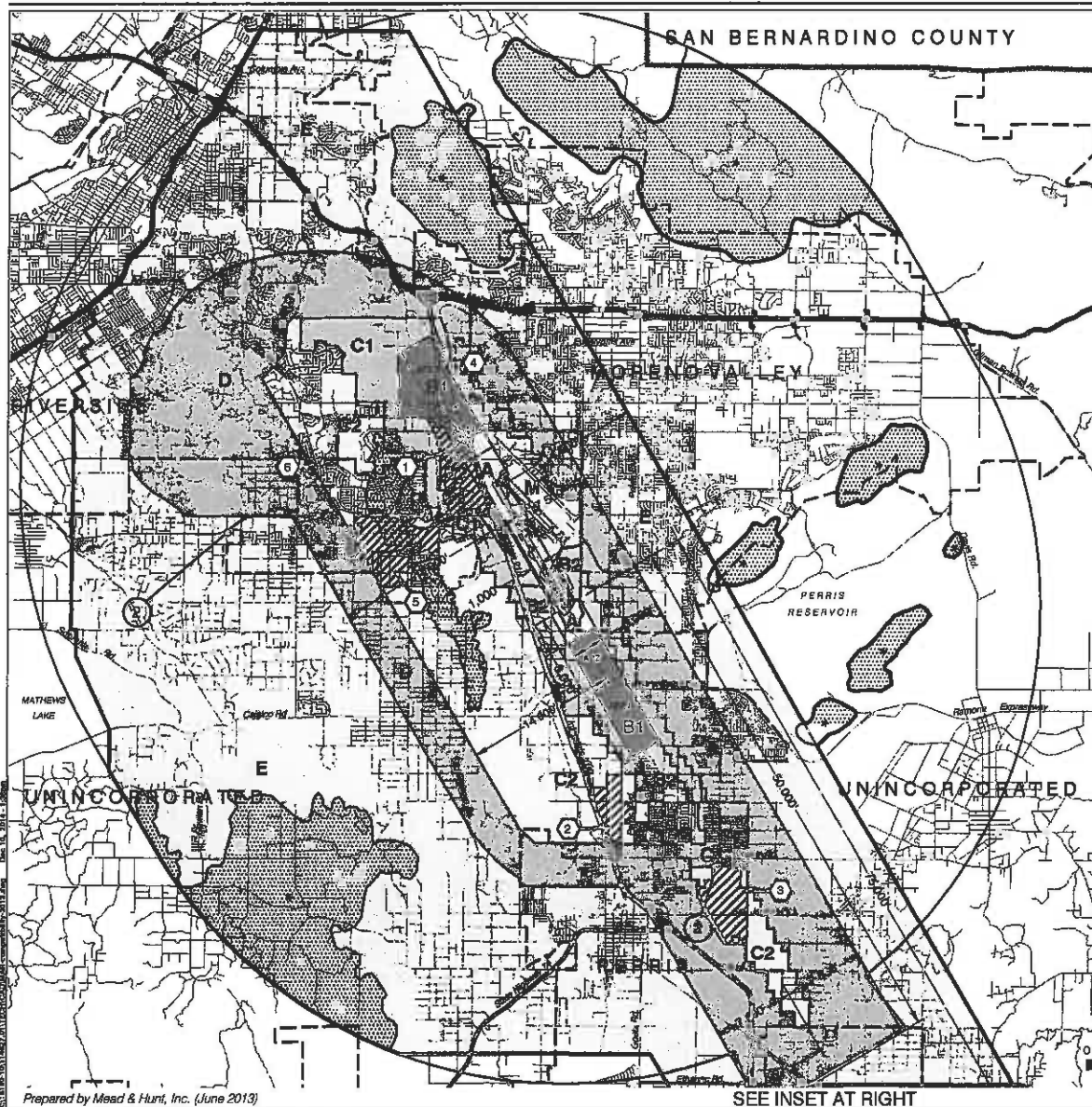
## **AIRPORT LAND USE COMMISSION**

cc: BLC Fleming LLC, Noah Shih (applicant/property owner)  
K&A Engineering, Inc. (representative)  
The Fleming Family Limited Partnership (property owner)  
Gary Gosliga, Airport Manager, March Inland Port Airport Authority  
Denise Hauser or Daniel Rockholt, March Air Reserve Base  
ALUC Case File

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# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



**LEGEND**

**Compatibility Zones**

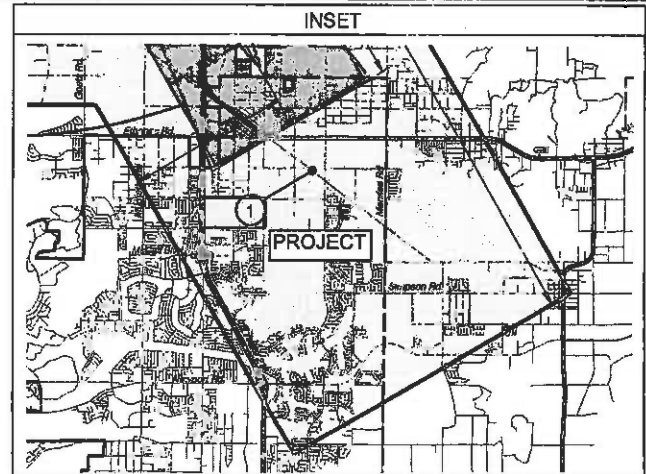
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

**Boundary Lines**

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- ▨ Site-Specific Exceptions (existing local agency commitments to development projects)

- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



Note:  
All dimensions are measured from runway ends and centerlines.



Base map source: County of Riverside 2013

**Riverside County  
Airport Land Use Commission  
March Air Reserve Base / Inland Port Airport  
Land Use Compatibility Plan  
(Adopted November 13, 2014)**

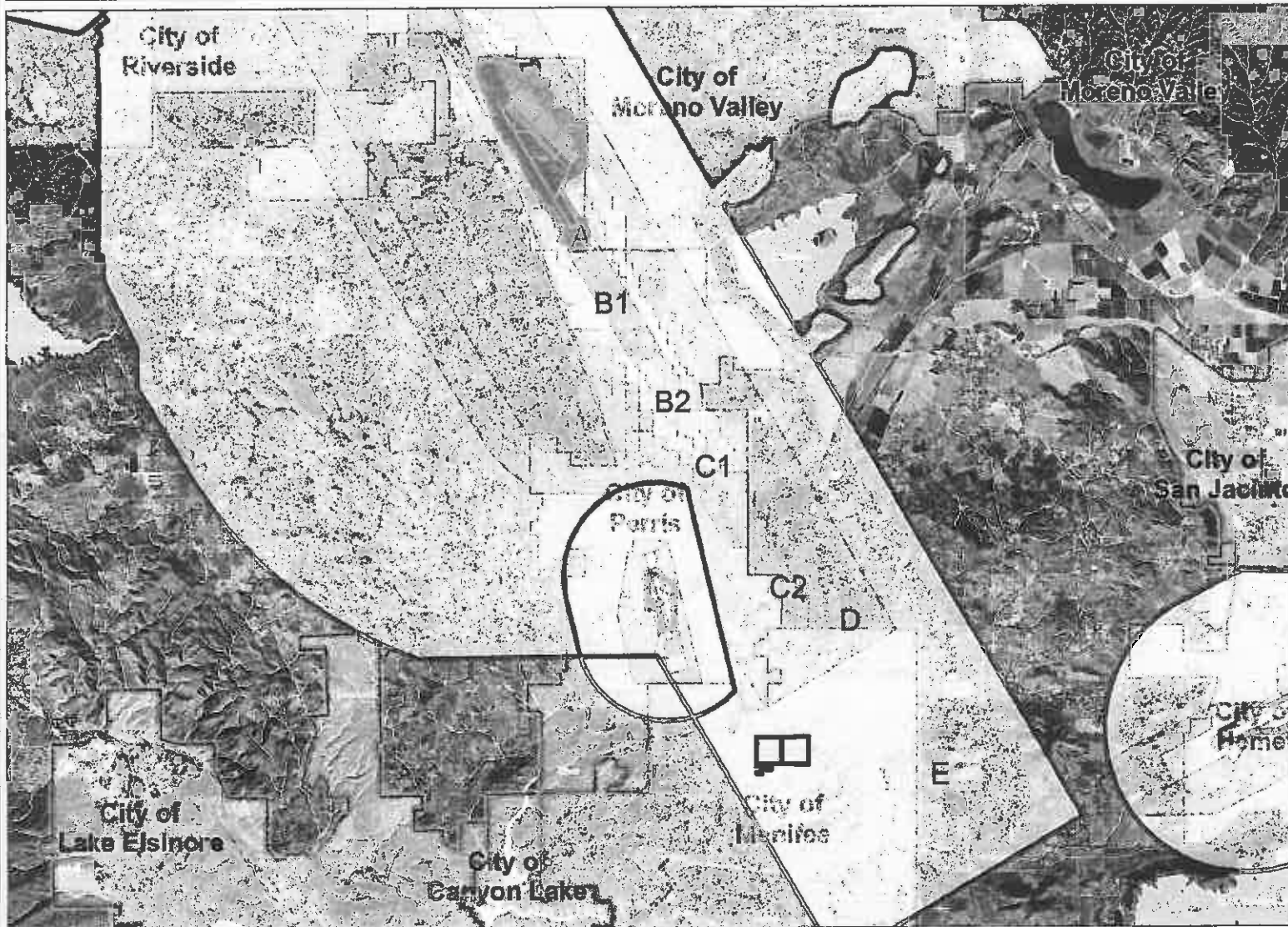
Map MA-1

**Compatibility Map**  
March Air Reserve Base / Inland Port Airport

SEE INSET AT RIGHT



# My Map



**Legend**

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT

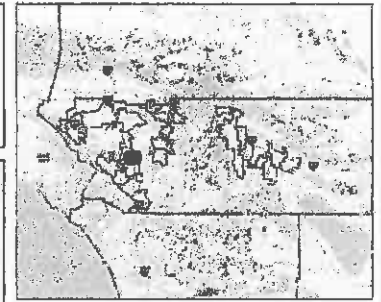
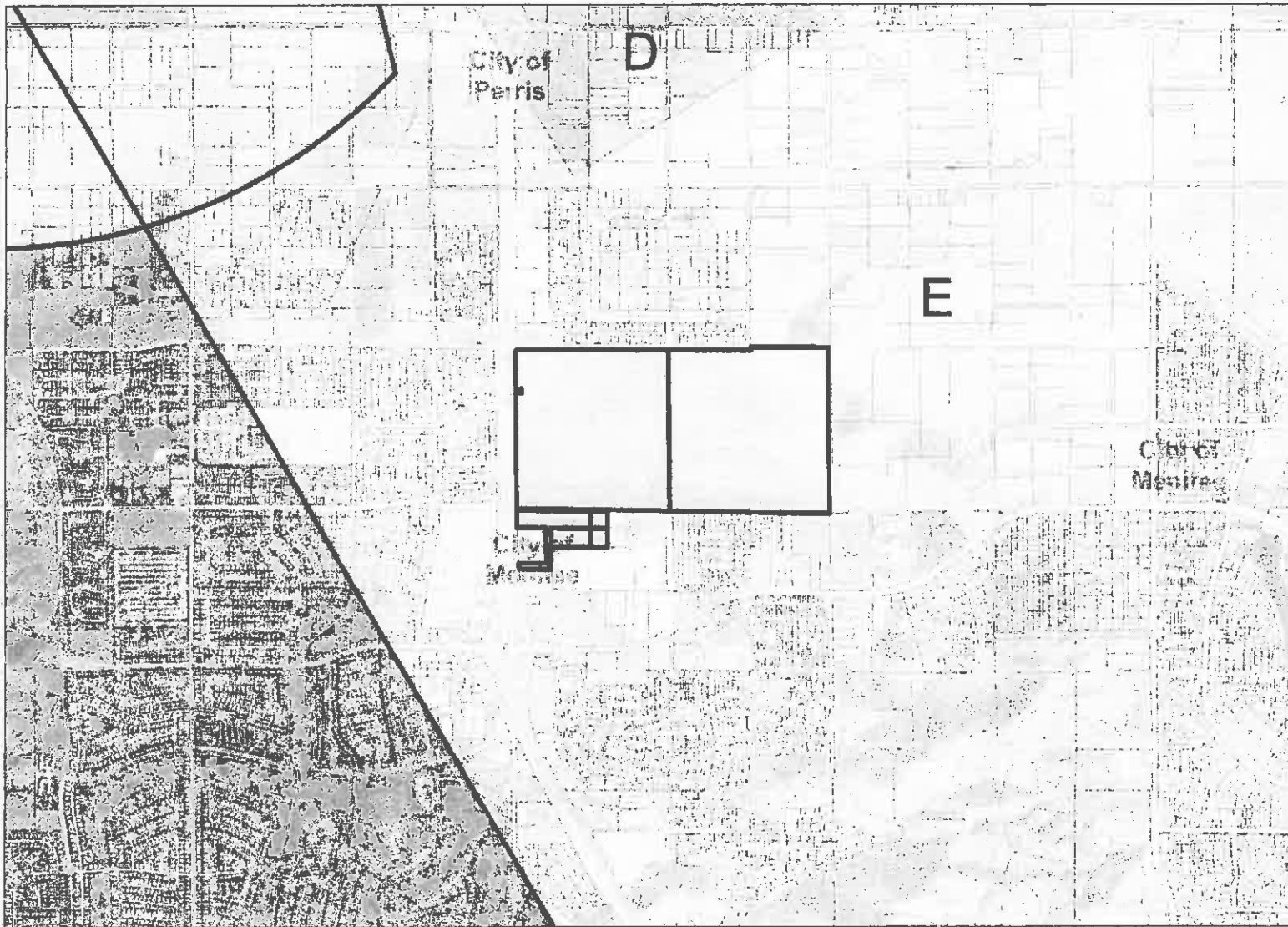


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**Notes**



# My Map



## Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



0 3,061 6,122 Feet



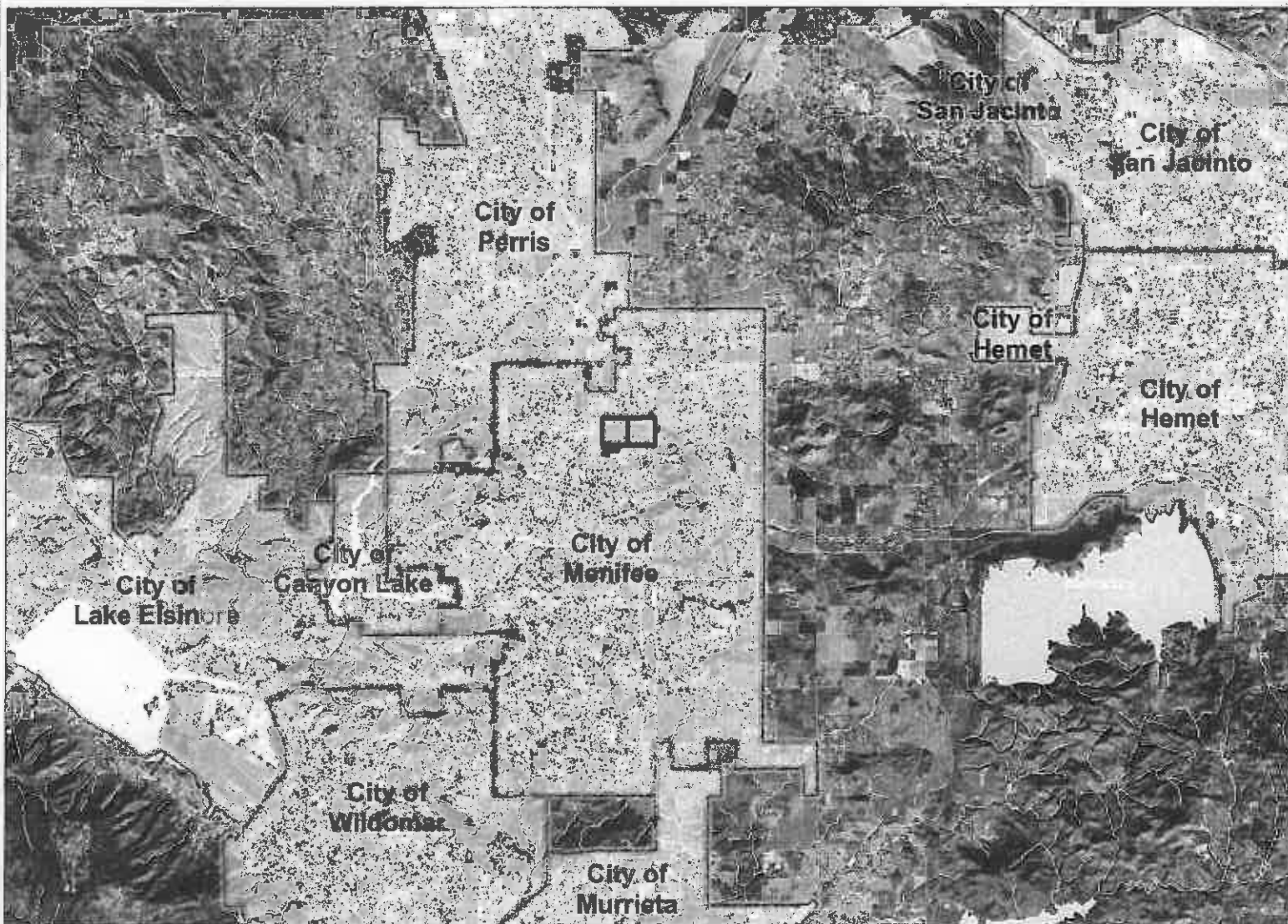
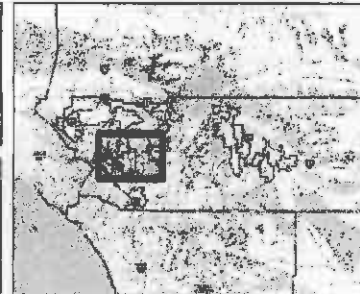
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## Notes

# My Map



**Legend**

- City Boundaries
- Cities**
- adjacent\_highways**
- Interstate
- Interstate 3
- State Highways; 60
- State Highways 3
- US HWY
- OUT
- highways\_large**
- HWY
- INTERCHANGE
- INTERSTATE
- USHWY
- counties
- cities



0 18,194 36,388 Feet



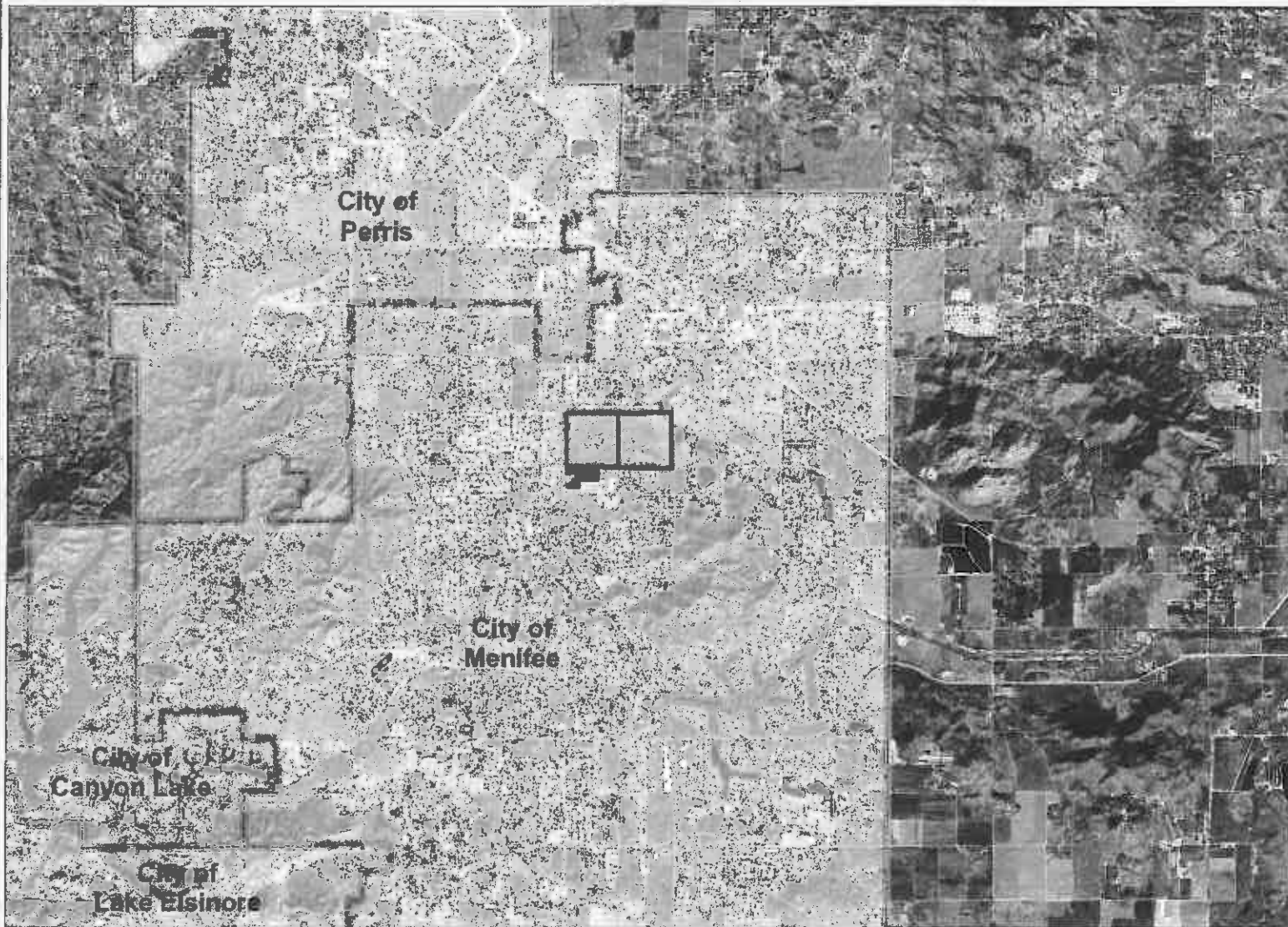
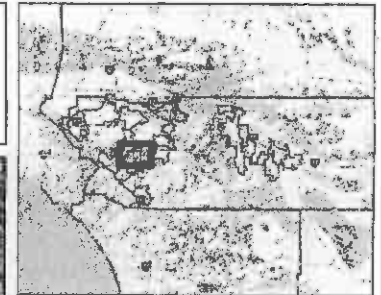
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## Notes

# My Map



## Legend

-  City Boundaries
- Cities**
- adjacent\_highways**
  -  Interstate
  -  Interstate 3
  -  State Highways: 60
  -  State Highways 3
  -  US HWY
  -  OUT
- highways\_large**
  -  HWY
  -  INTERCHANGE
  -  INTERSTATE
  -  US HWY
-  counties
-  cities



0 9,097 18,194 Feet



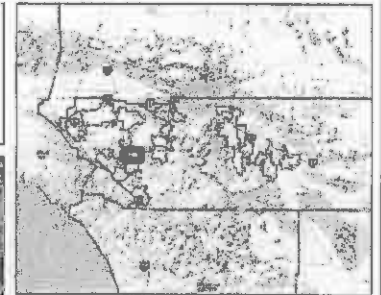
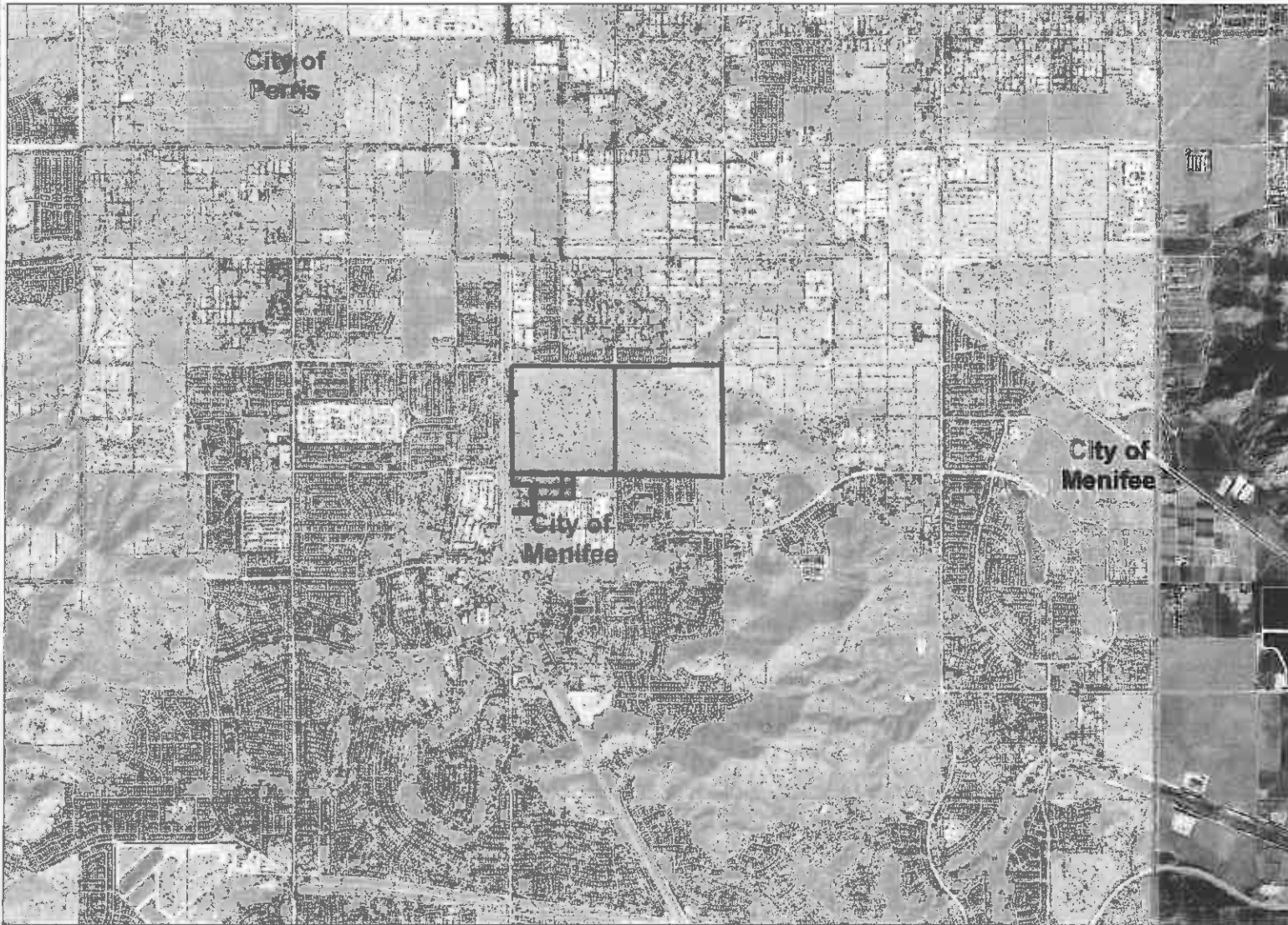
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## Notes

# My Map



## Legend

- City Boundaries
- Cities**
- highways**
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- majorroads
- counties
- cities
- hydrographylines**
- waterbodies**
- Lakes
- Rivers



0

4,548

9,097 Feet



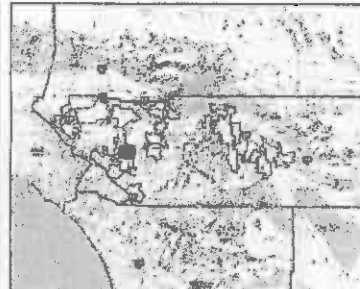
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## Notes

# My Map



## Legend

-  City Boundaries
- Cities**
- roads**
- highways**
-  HWY
-  INTERCHANGE
-  INTERSTATE
-  OFFRAMP
-  ONRAMP
-  USHWY
- roads**
-  Major Roads
-  Arterial
-  Collector
-  Residential
-  counties
-  cities
- hydrography**
- lines**
- waterbodies**
-  Lakes
-  Rivers



0 3,061 6,122 Feet



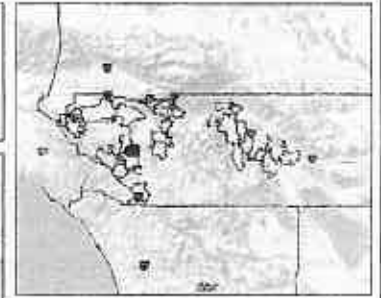
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## Notes

# My Map



## Legend

- City Boundaries
- Cities**
- roads**
- highways**
  - HWY
  - INTERCHANGE
  - INTERSTATE
  - OFFRAMP
  - ONRAMP
  - USHWY
- roads**
  - Major Roads
  - Arterial
  - Collector
  - Residential
- counties
- cities
- hydrography**
- waterbodies**
  - Lakes
  - Rivers



0 2,274 4,548 Feet



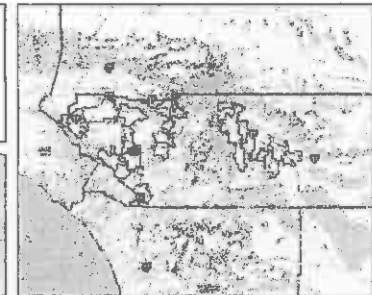
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## Notes

# My Map



## Legend

-  City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
-  counties
-  cities
- hydrographylines
- waterbodies
-  Lakes
-  Rivers



0 1,137 2,274 Feet



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## Notes



# TENTATIVE TRACT MAP NO. 37391

**ENGINEER**  
 K & A ENGINEERING, INC.  
 127 N. SHERRMAN STREET, SUITE 117  
 CORONA, CA 92680  
 PHONE: (951) 279-1800  
 PROJECT MANAGER: ANDY FULLER, P.E.

**APPLICANT**  
 BIC FLEASING LLC  
 100 BAYVIEW CIRCLE, SUITE 2200  
 NEWPORT BEACH, CA 92660  
 REPRESENTATIVE: MICHAEL SCULLING  
 PH: 949-945-2556

**PROPOSED ZONINGS**  
 SP: ZONE

**PROPOSED DEVELOPMENT**

PROPOSED DEVELOPMENT	ACRES
EDUCATIONAL	18.7 AC
MEDIUM DENSITY RESIDENTIAL	222.6 AC
OPEN SPACE/PREPARATION	18.1 AC
WATER QUALITY/RETENTION BASINS	8.3 AC
ROADWAYS	36.6 AC
<b>TOTAL AREAS</b>	<b>= 331 AC</b>

**OWNERS**  
 BIC FLEASING LLC  
 100 BAYVIEW CIRCLE, SUITE 2200  
 NEWPORT BEACH, CA 92660  
 ATTN: MICHAEL SCULLING  
 PH: 949-945-2556

**OWNERS**  
 THE FLEMING FAMILY LIMITED PARTNERSHIP  
 1600 WESTERN BLVD., SUITE 402  
 ENOCA, CA 97438  
 ATTN: BOB DUBINSKI  
 PH: 800-965-5000

**UTILITIES AND SERVICES**  
 AGENCY SERVING THE AREA: SOUTHERN CALIFORNIA Edison  
 ELECTRIC: SOUTHERN CALIFORNIA Edison  
 GAS: SOUTHERN CALIFORNIA GAS CO.  
 TELEPHONE: FRONTIER COMMUNICATIONS  
 CABLE TV: TIME WARNER  
 WATER: EASTERN MUNICIPAL WATER DISTRICT  
 SEWER: EASTERN MUNICIPAL WATER DISTRICT  
 SCHOOL DIST: PENINSULA UNIFIED SCHOOL DISTRICT

**PROPERTY ACREAGE**  
 TOTAL RAW GROSS 331.01 AC

**ASSESSOR'S PARCEL NUMBER**  
 333-020-028, 010 AND 333-030-012, 013, 021 & 022

**SECTIONS, TOWNSHIPS AND RANGES**  
 NORTH 1/4 AND A POR. OF SW QUARTER OF SEC. 22, T2S, R15E, S24E.

**THOMAS BROTHERS MAP**  
 THOMAS GUIDE RIVERSIDE (2007) PAGE 838

**EXISTING GENERAL PLAN DESIGNATIONS**  
 FLEASING RAMON SP

**PRESENT USE**  
 VACANT

**PROPOSED USE**  
 RESIDENTIAL

**EXISTING ZONING**  
 SINGLE FAMILY DWELLINGS (R-1)  
 SCenic HIGHWAY COMMERCIAL (S-C-P-5)

**BENCHMARK**  
 THE BASIS OF BEARINGS FOR THIS SURVEY IS THE CALIFORNIA STATE PLANE COORDINATE SYSTEM, CCS83, ZONE 6, BASED LOCALLY ON CONTROL STATIONS "T998", "T999" AND "T91".  
 HORIZONTAL ANGLES (SLOTTED PRISM) AS SHOWN HEREON, ALL BEARINGS SHOWN ON THIS MAP ARE TRUE, SLOTTED BEARINGS AND DISTANCES FROM REFERENCE MAPS OR RECORDS ARE AS SHOWN PER THAT RECORD. REFERENCE ALL DISTANCES SHOWN ARE GROUND DISTANCES UNLESS SPECIFIED OTHERWISE. GRID DISTANCES MAY BE OBTAINED BY MULTIPLYING THE GROUND DISTANCE BY A CORRECTION FACTOR OF 0.9999974. CALCULATIONS ARE MADE AT POINT NUMBER 1 WITH COORDINATES OF: N: 2211450.38, E: 828264.26, USING AN ELEVATION OF 1443.59 FEET.

CONVERGENCE ANGLE AT POINT #1 = -0°30'19.28"

**PROPOSED IMPROVEMENT SCHEDULE**  
 SCHEDULE "D"

**PROPOSED STORM DRAINS**  
 PROPOSED DRAINAGE & STORM DRAIN SYSTEMS WILL BE CONSTRUCTED WITH SUBSIDIARY STORM DRAIN SYSTEMS SHALL BE MAINTAINED BY R.C.F.C. OR CITY OF MENIFEE

**GEOLOGIC HAZARD NOTE**  
 PROJECT SITE IS NOT SUBJECT TO LIQUEFACTION GEOLOGIC HAZARDS, ALGUESI-PROBLE OF SPECIAL STUDIES ZONE

**COMMUNITY SERVICES DISTRICT**  
 THIS PROJECT WILL BE SERVED BY C.F.D. NO. 2015-2

**WELLS**  
 THERE ARE NO KNOWN EXISTING WELLS ON SITE OR WITHIN 300 FEET OF PROPERTY

**AERIAL TOPOGRAPHY**  
 2012 UNITS & ASSOCIATES  
 2012 MANUUT AVENUE E  
 RUSTON, CA 92780  
 PH: 714-823-2027

**PROPOSED SUBSURFACE SEPTIC DISPOSAL**  
 THERE IS NO SUBSURFACE SEPTIC DISPOSAL PROPOSED FOR THIS PROJECT

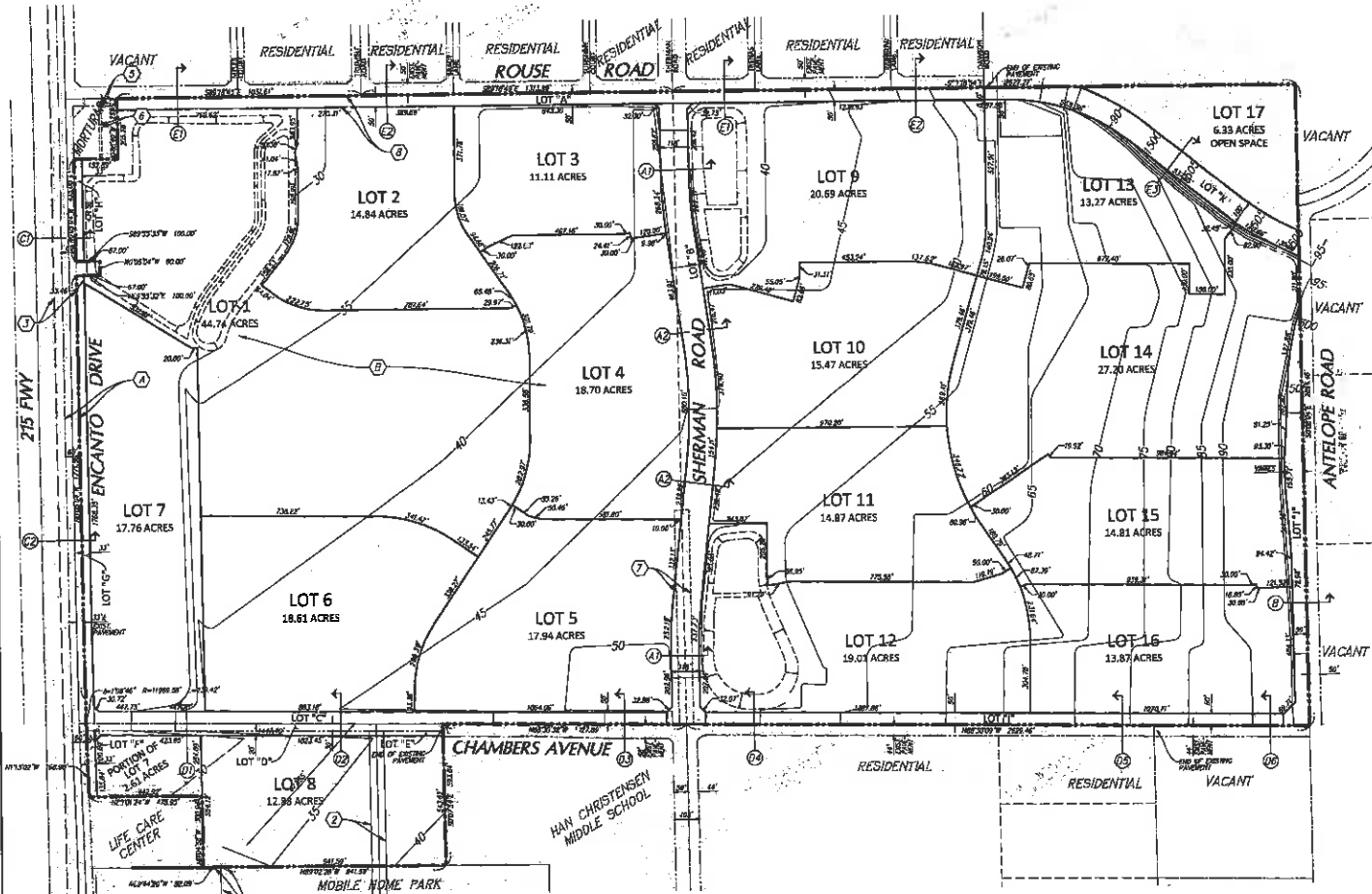
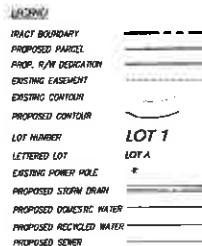
**MOBILE HOME AND RV PARKING**  
 THERE IS NO DESIGNATED MOBILE HOME OR R.V. PARKING FACILITIES PROPOSED FOR THIS PROJECT

**FLOOD PLAIN DESIGNATION**  
 ZONE X OR AREAS OUTSIDE THE 100-YEAR FLOOD PLAIN

**LOT SUMMARY**

LOT	GROSS SQ. ACRES	DESC.
1	1484.04	RESIDENTIAL/BLASH
2	640.50	RESIDENTIAL
3	483.01	RESIDENTIAL
4	844.27	RESIDENTIAL
5	104.66	RESIDENTIAL
6	810.81	RESIDENTIAL
7	887.57	COMMERCIAL
8	961.02	PARK
9	861.28	RESIDENTIAL/BASIN
10	878.85	RESIDENTIAL
11	847.77	RESIDENTIAL
12	818.97	RESIDENTIAL/BLASH
13	129.64	RESIDENTIAL
14	1214.83	RESIDENTIAL
15	645.12	RESIDENTIAL
16	604.77	RESIDENTIAL
17	276.26	OPEN SPACE
A	288.11	BLASH
B	212.81	PUBLIC ROAD
C	120.83	PUBLIC ROAD
D	48.40	EX. PUBLIC ROAD
E	15.88	PUBLIC ROAD
F	5.88	PUBLIC ROAD
G	90.79	PUBLIC ROAD
H	54.14	PUBLIC ROAD
I	133.88	PUBLIC ROAD
J	128.27	PUBLIC ROAD
K	137.49	PUBLIC ROAD
TOTAL	24414.96	281.61

**TRACT BOUNDARY**  
 PROPOSED PARCEL  
 PROP. E/W DESIGNATION  
 EXISTING EASEMENT  
 EXISTING CONTAIN  
 PROPOSED CONTAIN  
 LOT NUMBER  
 LETTERED LOT  
 EXISTING POWER POLE  
 PROPOSED STORM DRAIN  
 PROPOSED DOMESTIC WATER  
 PROPOSED RECYCLED WATER  
 PROPOSED SEWER



**LEGAL DESCRIPTION**  
 IN THE CITY OF MENIFEE, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, BEING A PORTION OF THE NORTHERN QUARTER OF SECTION 22, TOWNSHIP 5 SOUTH, RANGE 3 WEST, S24E, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:  
 BEGINNING AT THE WEST NORTHEASTLY CORNER OF SAID SECTION 22; THENCE ALONG THE EASTERN LINE OF SAID SECTION, SAID CORNER BEING 204.64 FEET TO THE CENTERLINE OF CHAMBERS ROAD (44.50 FEET HALF WIDTH); THENCE ALONG SAID CENTERLINE OF CHAMBERS ROAD TO THE CENTERLINE OF ENCANTO ROAD AND SHERMAN ROAD; THENCE CONTINUING ALONG SAID CENTERLINE NORTH 89° 30' 30" WEST, 1022.89 FEET; THENCE LEAVING SAID CENTERLINE NORTH 89° 30' 30" WEST, 333.64 FEET; THENCE NORTH 89° 00' 00" WEST, 54.50 FEET; THENCE NORTH 89° 00' 00" WEST, 82.00 FEET; THENCE NORTH 89° 00' 00" WEST, 188.43 FEET; THENCE NORTH 89° 00' 00" WEST, 188.43 FEET TO THE EASTERN RIGHT-OF-WAY LINE OF ENCANTO DRIVE (44.50 FEET HALF WIDTH); THENCE ALONG SAID LINE CENTERLINE NORTH 89° 30' 30" WEST, 253.00 FEET TO THE BEGINNING OF A TANGENT CURVE, CORNER EASTERLY, HAVING A RADIUS OF 1789.36 FEET; THENCE ALONG SAID CURVE 253.00 FEET THROUGH A CENTRAL ANGLE OF 87° 30' 45"; THENCE NORTH 89° 00' 00" WEST, 177.06 FEET; THENCE LEAVING SAID RIGHT-OF-WAY LINE NORTH 89° 30' 30" EAST, 1022.89 FEET; THENCE NORTH 89° 00' 00" WEST, 82.00 FEET; THENCE NORTH 89° 00' 00" WEST, 188.43 FEET TO THE EASTERN RIGHT-OF-WAY LINE OF ENCANTO DRIVE (44.50 FEET HALF WIDTH); THENCE ALONG SAID RIGHT-OF-WAY LINE NORTH 89° 00' 00" WEST, 453.00 FEET; THENCE LEAVING SAID LINE CENTERLINE NORTH 89° 30' 30" WEST, 1022.89 FEET; THENCE NORTH 89° 00' 00" WEST, 253.00 FEET TO THE CENTERLINE OF ENCANTO DRIVE (44.50 FEET HALF WIDTH); THENCE CONTINUING ALONG SAID CENTERLINE SOUTH 89° 14' 45" WEST, 1023.16 FEET TO THE CENTERLINE INTERSECTION OF ENCANTO ROAD AND SHERMAN ROAD; THENCE CONTINUING ALONG SAID CENTERLINE SOUTH 89° 14' 45" WEST, 133.91 FEET TO THE CENTERLINE INTERSECTION OF ENCANTO ROAD AND SHERMAN ROAD; THENCE CONTINUING ALONG SAID LINE SOUTH 89° 14' 45" WEST, 292.72 FEET TO THE POINT OF BEGINNING.

CONTAINING 13.61 ACRES, MORE OR LESS

**EXISTING EASEMENT NOTES**

- EASEMENTS FOR THE PURPOSES SHOWN BELOW AND RIGHTS INCIDENTAL THERETO AS SHOWN BY A DOCUMENT:  
 GRANTED TO SOUTHERN CALIFORNIA Edison COMPANY, A CORPORATION PUBLIC UTILITIES  
 RECORDING DATE: JANUARY 11, 1995  
 RECORDING NO. 216 (PARTIAL)  
 AFFECTS: SAID LAND MORE PARTICULARLY DESCRIBED HEREIN.  
 THE EASEMENT IS BLANKET IN NATURE AND COVERS ENTIRE PROPERTY.
- EASEMENTS FOR THE PURPOSES SHOWN BELOW AND RIGHTS INCIDENTAL THERETO AS SHOWN BY A DOCUMENT:  
 PUBLIC UTILITY PUBLIC SERVICE AND DEMANDS FACILITIES  
 RECORDING DATE: JANUARY 11, 1995  
 RECORDING NO. 216 (PARTIAL)  
 AFFECTS: SAID LAND MORE PARTICULARLY DESCRIBED HEREIN.
- THE CONVEYOR OF SAID LAND DOES NOT INCLUDE RIGHTS OF ACCESS TO OR FROM THE STREET, NEITHER ADJUTANT SAID LAND, SAID RIGHTS INCLUDING ANY EGRESS.  
 RECORDING DATE: SEPTEMBER 8, 1993  
 RECORDING NO. 216 (PARTIAL)  
 AFFECTS: SAID LAND MORE PARTICULARLY DESCRIBED HEREIN.
- EASEMENTS FOR THE PURPOSES SHOWN BELOW AND RIGHTS INCIDENTAL THERETO AS SHOWN BY A DOCUMENT:  
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 RECORDING DATE: JANUARY 11, 1995  
 RECORDING NO. 216 (PARTIAL)  
 AFFECTS: SAID LAND MORE PARTICULARLY DESCRIBED HEREIN.
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 RECORDING DATE: JANUARY 11, 1995  
 RECORDING NO. 216 (PARTIAL)  
 AFFECTS: SAID LAND MORE PARTICULARLY DESCRIBED HEREIN.

**EXISTING EASEMENT NOTES - (CONT.)**

- EASEMENTS FOR THE PURPOSES SHOWN BELOW AND RIGHTS INCIDENTAL THERETO AS SHOWN BY A DOCUMENT:  
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 RECORDING NO. 216 (PARTIAL)  
 AFFECTS: SAID LAND MORE PARTICULARLY DESCRIBED HEREIN.



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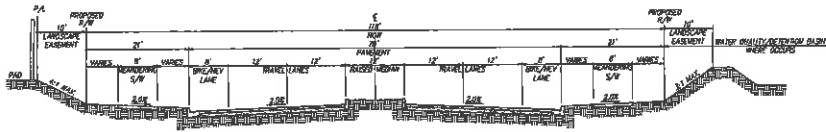
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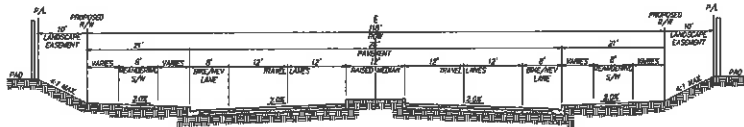
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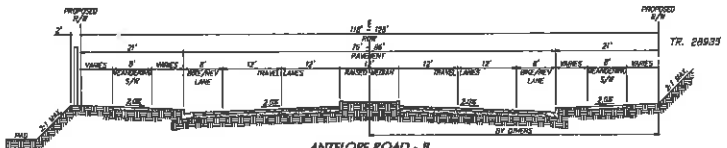
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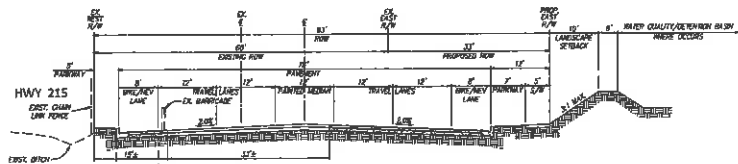
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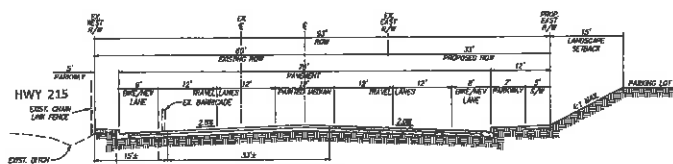
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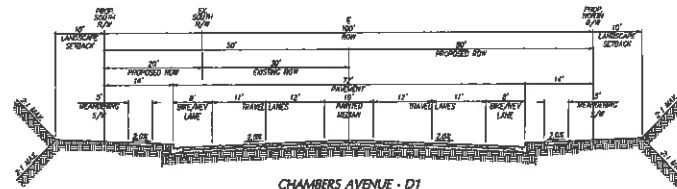
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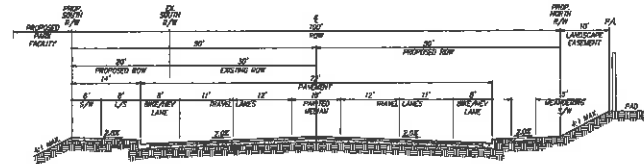
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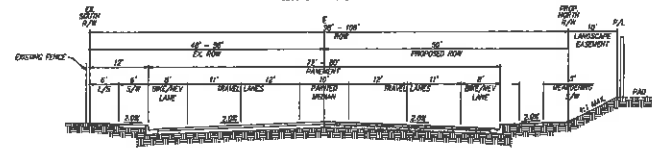
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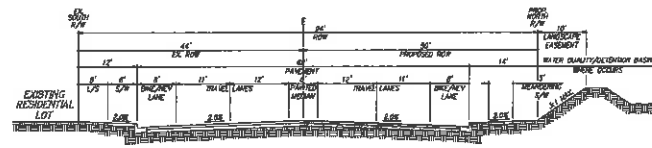
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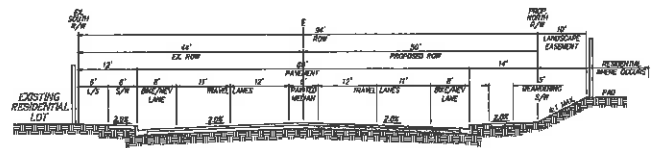
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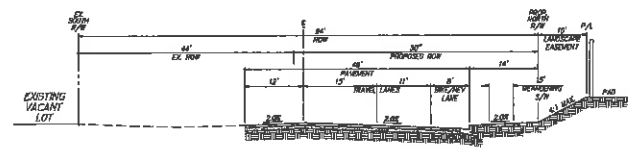
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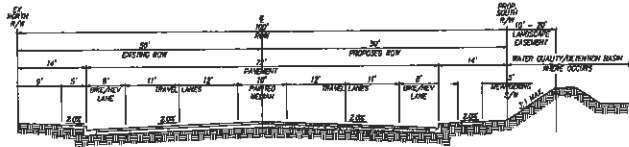
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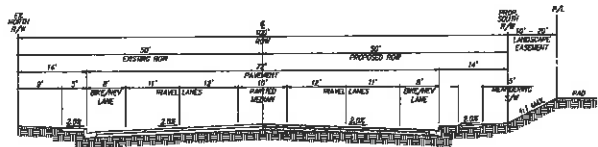
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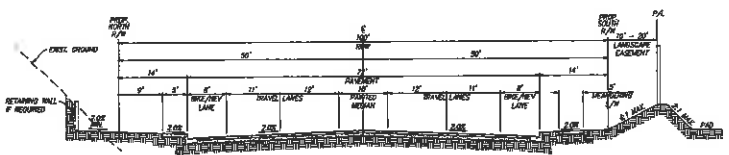
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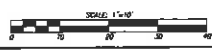
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Sheet No. 3 of 3

# FLEMING RANCH SPECIFIC PLAN

As shown in Figure 1.6: Menfee Zoning Map 2017, the existing zoning of the site is predominately One-Family Dwellings (R-1) with the portion adjacent to

Encanto Drive zoned Scenic Highway Commercial (C-P-S). This SP changes the zoning to SP Zone. (See Figure 1.7: Menfee Zoning Map as Amended).

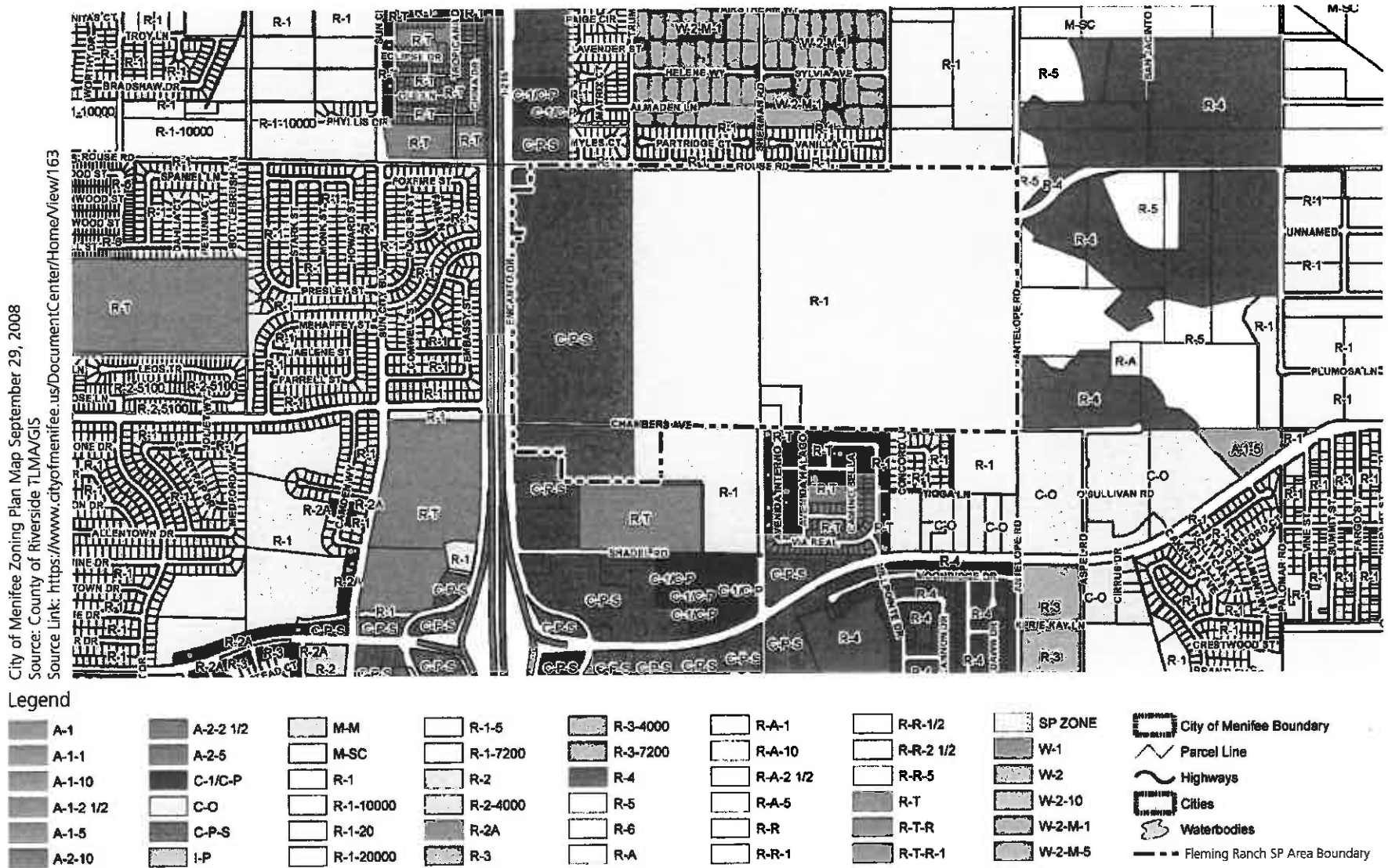
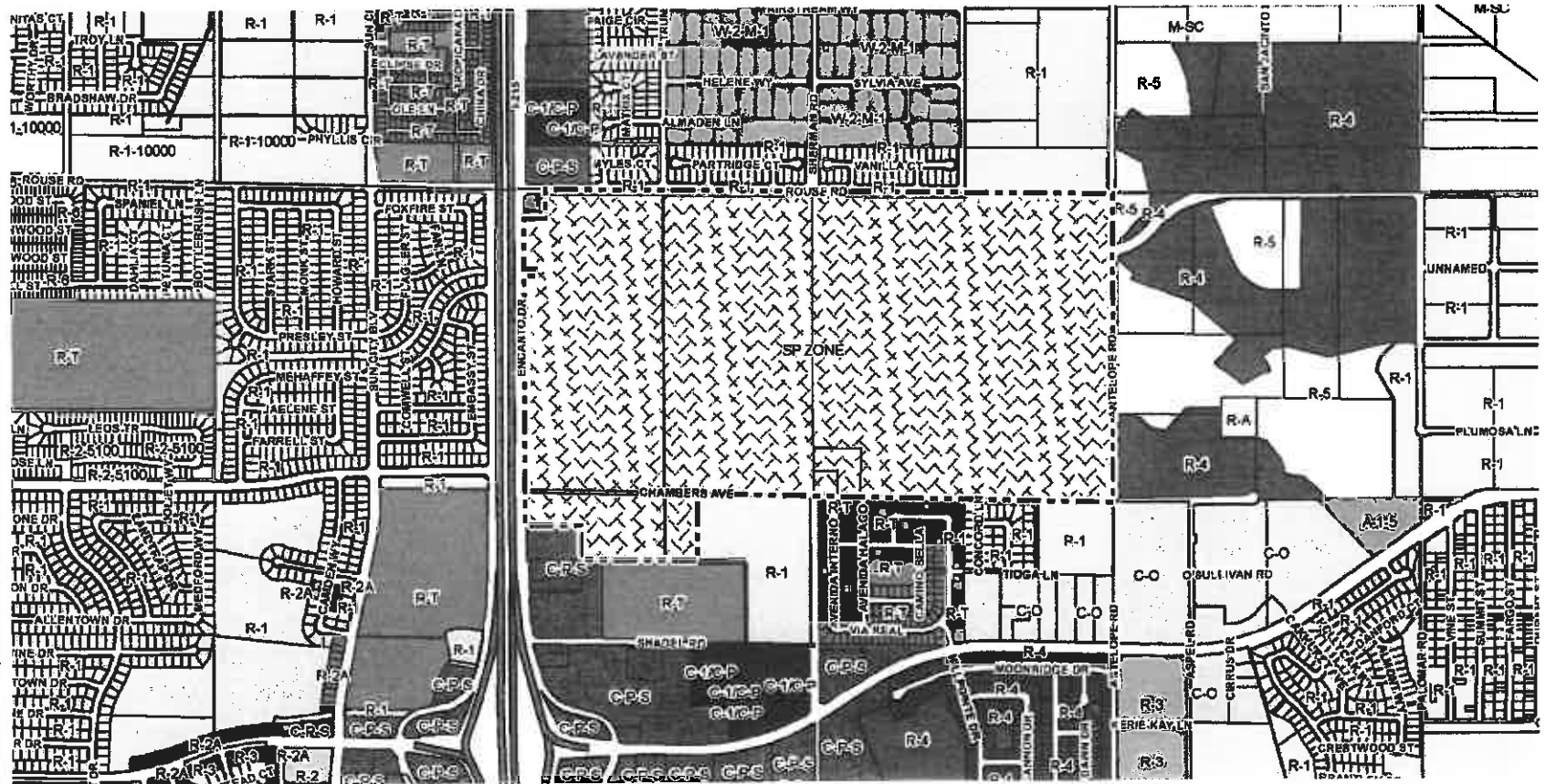


FIGURE 1.6: MENFEE ZONING MAP 2017

City of Menifee Zoning Plan Map September 29, 2008  
 Source: County of Riverside TLMA/GIS  
 Source Link: <https://www.cityofmenifee.us/DocumentCenter/Center/Home/View/163>



Legend

A-1	A-2-2 1/2	M-M	R-1-5	R-3-4000	R-A-1	R-R-1/2	SP ZONE	City of Menifee Boundary
A-1-1	A-2-5	M-SC	R-1-7200	R-3-7200	R-A-10	R-R-2 1/2	W-1	Parcel Line
A-1-10	C-1/C-P	R-1	R-2	R-4	R-A-2 1/2	R-R-5	W-2	Highways
A-1-2 1/2	C-O	R-1-10000	R-2-4000	R-5	R-A-5	R-T	W-2-10	Cities
A-1-5	C-P-S	R-1-20	R-2A	R-6	R-R	R-T-R	W-2-M-1	Waterbodies
A-2-10	I-P	R-1-20000	R-3	R-A	R-R-1	R-T-R-1	W-2-M-5	Fleming Ranch SP Area Boundary

FIGURE 1.7: MENIFEE ZONING MAP AS AMENDED



**FLEMING RANCH**  
**SPECIFIC PLAN**  
SP2017-187

**SUBMITTAL DATE:**  
**AUGUST 2017**

**CITY OF MENIFEE**

# **FLEMING RANCH**

**SPECIFIC PLAN  
SP2017-187**

**AUGUST 2017**

**BLC FLEMING LLC**

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- Table A: Community Plant Palette ..... A-2

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# SECTION 1



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## INTRODUCTION

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This Section provides a context for the Fleming Ranch Specific Plan (SP). The existing conditions, land uses, opportunities and constraints are described. It concludes with a description of the SP document purpose, accompanying actions, and a statement of consistency with governmental land uses, polices and codes.

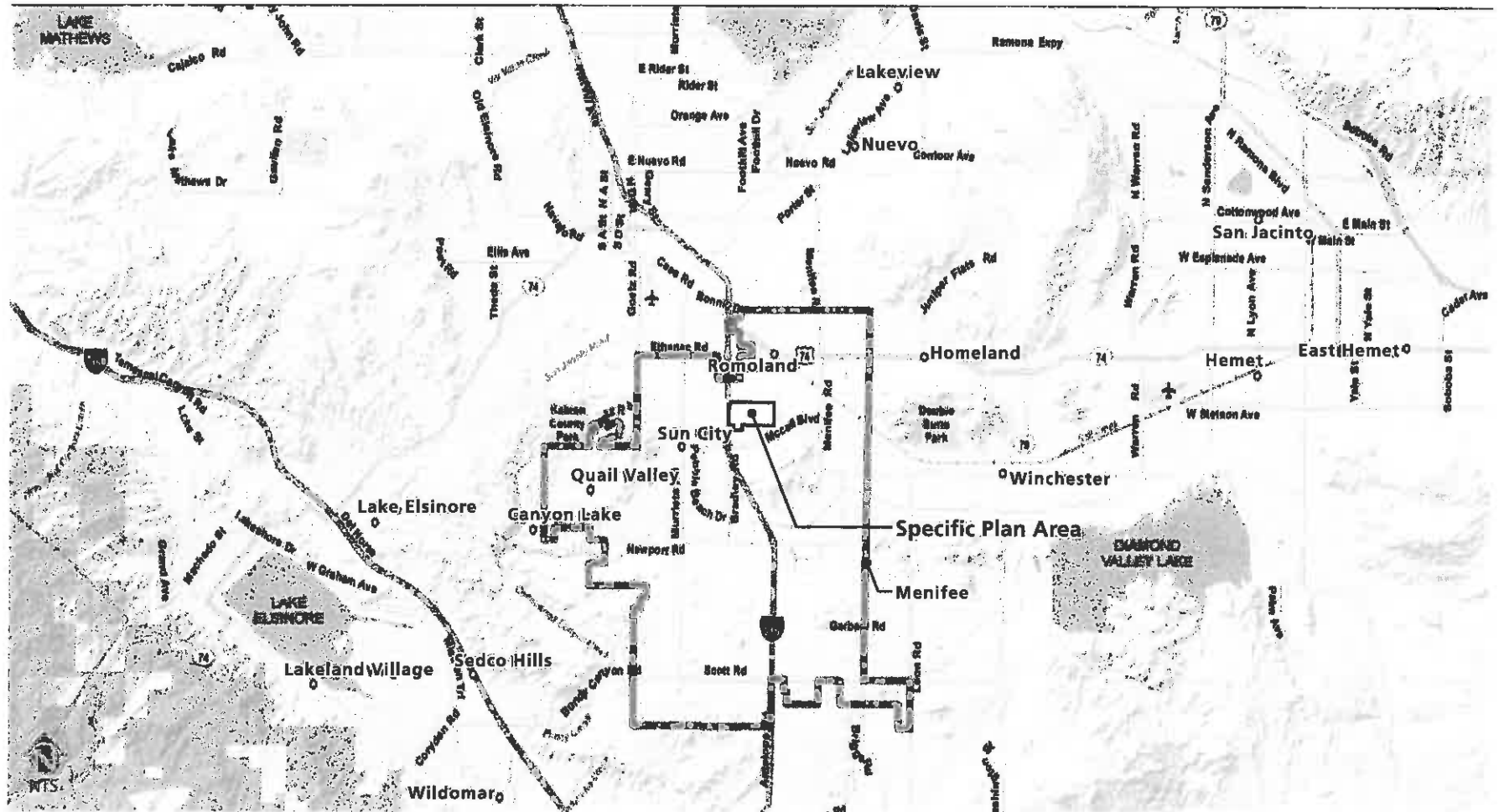
## **1.1 SP SETTING**

The first step in the planning process involves identifying the SP area's context within the surrounding community and region. This requires an analysis of the physical, cultural and political issues that affect the SP area.

### **1.1.1 LOCATION, CONTEXT AND JURISDICTION**

The SP consists of approximately 331 acres of land within the City of Menifee in Western Riverside County. (See Figure 1.1: Regional Map). The SP area is surrounded primarily by existing development (approximately 73% of perimeter) and can therefore be categorized as infill property. The remaining property surrounding the SP area consists of approved tract maps and open space.

Escondido Freeway, Interstate Highway 215 (I-215) to the west of the SP area, is the primary regional north-south transportation corridor within the western Riverside County region. The primary east-west regional transportation corridor is State Highway 74 (SR-74) located 1.5 miles north of the SP area, providing access to Interstate Highway 15 (I-15) to the west and the City of Hemet to the east. West of I-215 lies the master planned community of Sun City, founded in the early 1960s. Sun City is a four-square-mile, age-restricted community that consists of homes; two public golf courses, two recreation centers and a commercial center. Three significant water courses define the region: the San Jacinto River, Salt Creek and the Perris Valley Storm Drain. Storm water leaving the Fleming Ranch property eventually drains into Salt Creek which flows in a westerly direction into Canyon Lake.



**FIGURE 1.1: REGIONAL MAP**

The SP area is surrounded by a variety of developments and vacant land. Rouse Road and single-family residential homes are located to the north. On the east, there are flat, vacant land and rolling hills. Vacant land, single-family residential homes, the Hans Christensen Middle School, a mobile home park, a motel and a convalescent home are located to the south across Chambers Avenue. The western edge of the SP area includes Encanto Drive and the I-215 Freeway. In addition, a mortuary occupies a small parcel at the northwest corner of the property outside of the SP area. (See Figure 1.2: SP Area and Surrounding Development Map).

At the date this SP was adopted, Encanto Drive, Rouse Road and Chambers Avenue existed as partially built roadways that serve various properties and

developments that surround the site. Sherman Road was improved north and south of the SP area. A majority of the site was used historically for farming and grazing. (See Figure 1.3A: Panoramic Photos, Figure 1.3B: Panoramic Photos, Figure 1.3C: Panoramic Photos, and Figure 1.3D: Panoramic Photos.)

A majority of the undeveloped land uses surrounding the SP area lies within approved Tentative Tract Maps (TTMs). TTM 29777 is located along northeastern edge and allows for the development of 173 residential dwelling units, 11.6 acres of open space and 2.7 acres of recreational/park use. TTM 29835 is located along the eastern edge and allows for the development of 543 residential dwelling units, 71.6 acres of open space and 9.4 acres of recreational/park use.



**Legend**

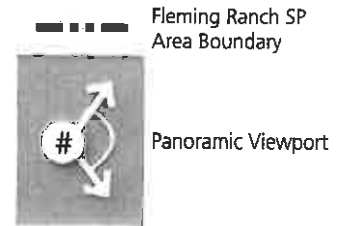
--- Fleming Ranch SP Area Boundary

**FIGURE 1.2: SP AREA AND SURROUNDING DEVELOPMENT MAP**

Key Map



Legend



*Panoramic of Rouse Road & Antelope Road (Extension) - Facing Southeast*



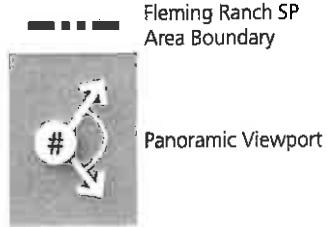
*Panoramic of Rouse Road & Sherman Road - Facing South*

**FIGURE 1.3A: PANORAMIC PHOTOS**

Key Map



Legend



*Panoramic of Rouse Road & Encanto Drive - Facing Southeast*



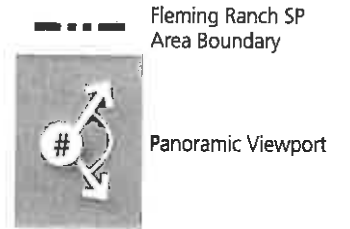
*Panoramic of Encanto Drive & the Convalescent Home - Facing North to Northeast*

**FIGURE 1.3B: PANORAMIC PHOTOS**

Key Map



Legend



*Panoramic of Chamber Avenue & Sherman Road - Facing North*



*Panoramic of the End of Chambers Avenue - Facing South to Northeast*

**FIGURE 1.3C: PANORAMIC PHOTOS**



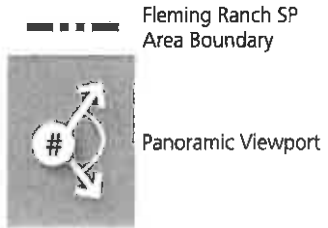
# SECTION 1

## INTRODUCTION

Key Map



Legend



*Panoramic from Chambers Avenue - Facing North*



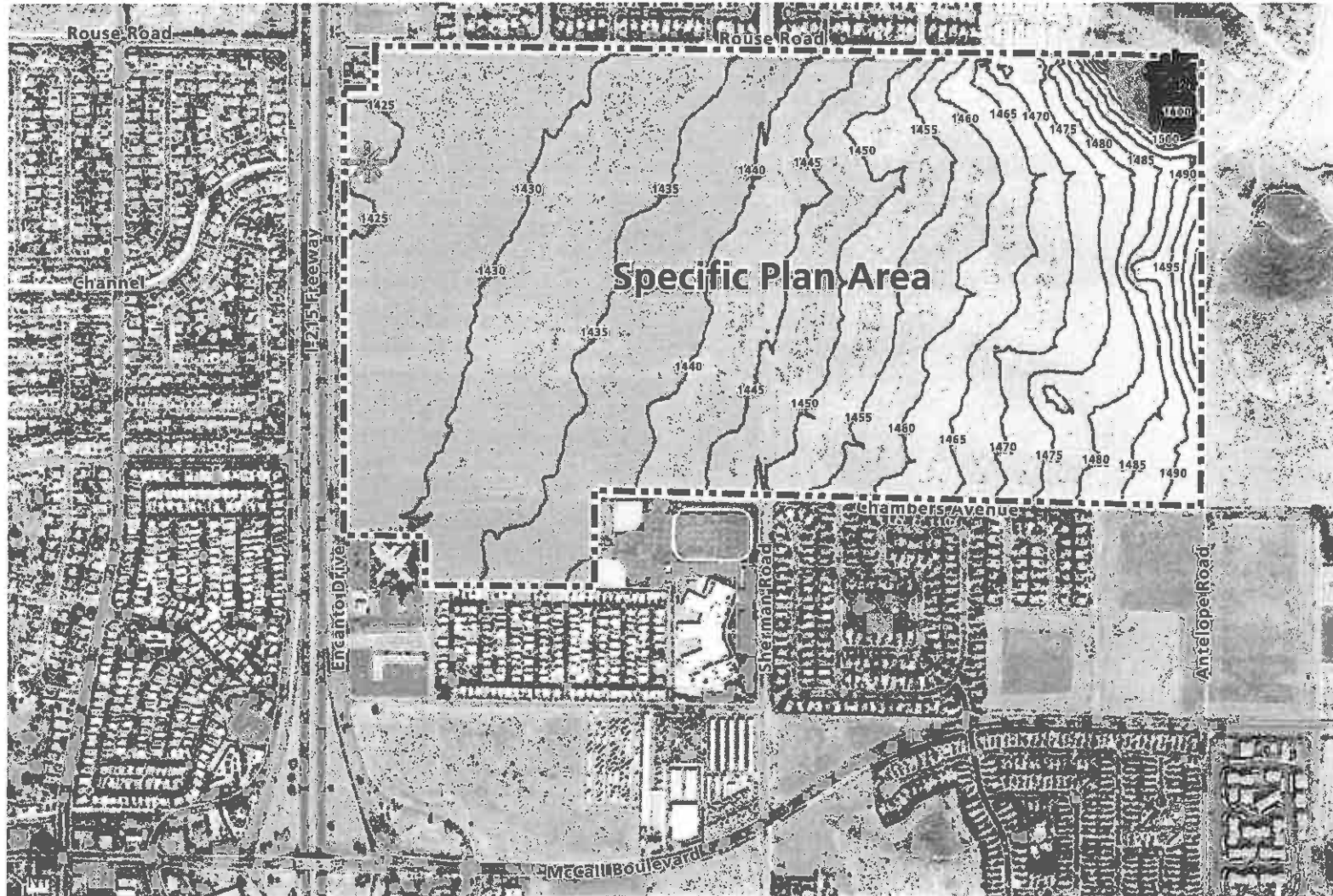
*Panoramic of Chamber Avenue & Antelope Road (Extension) - Facing Northwest*

**FIGURE 1.3D: PANORAMIC PHOTOS**

# FLEMING RANCH SPECIFIC PLAN

The land that makes up the SP area consists of mostly flat terrain with a prominent knoll located in the northeast corner. (See Figure 1.4: Existing Topography). The knoll provides the highest elevation on-site at 1,650 feet above mean sea level (AMSL). The lowest elevation

within the SP area is at 1,425 feet AMSL and is located near Encanto Drive in the northwest portion of the site. A majority of the storm water flows from east to west, passing under I-215 via a box culvert.



**FIGURE 1.4: EXISTING TOPOGRAPHY**

**Legend**

----- Fleming Ranch SP Area Boundary

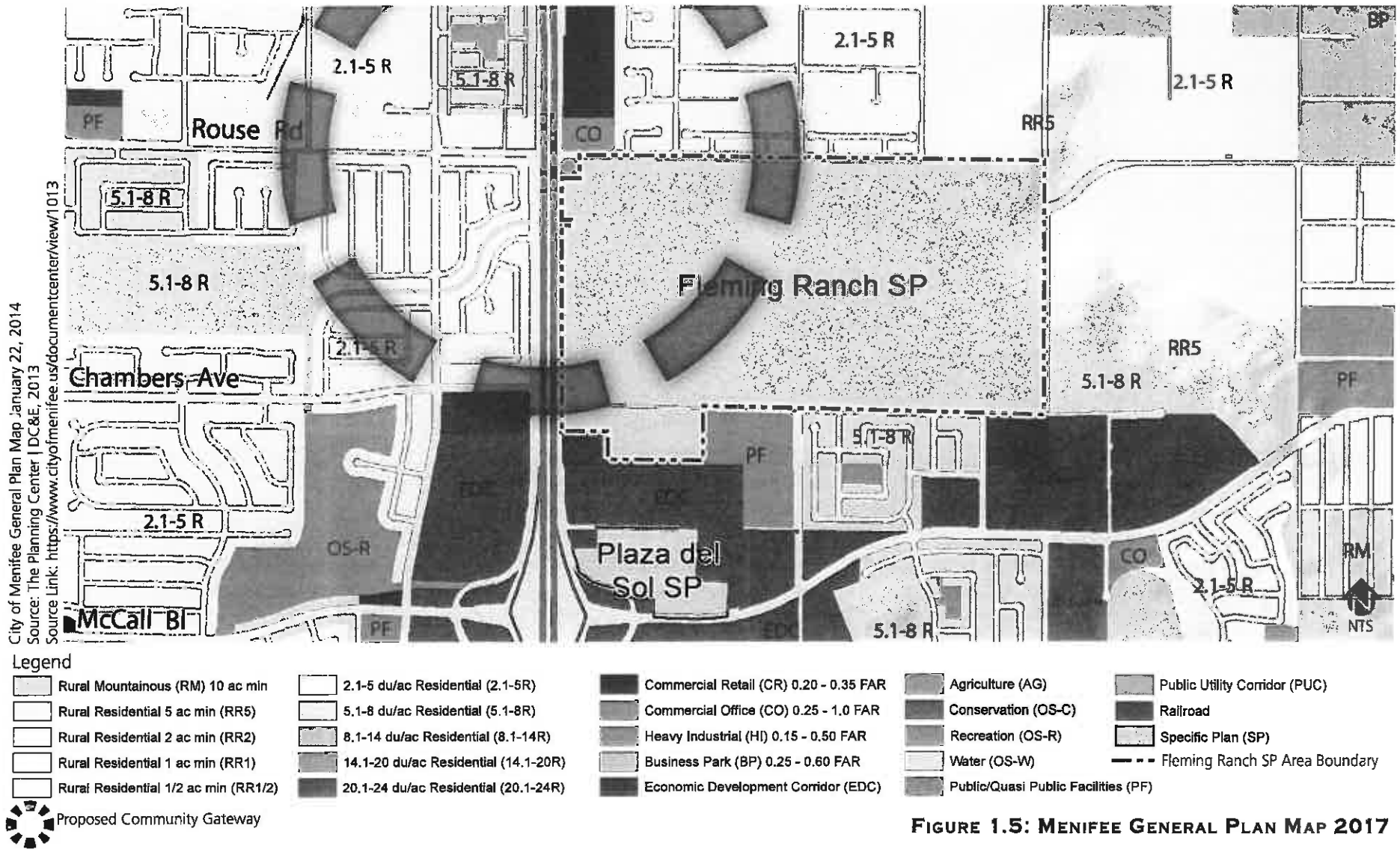
★ High Point (1,650')

★ Low Point (1,425')

# SECTION 1 INTRODUCTION

The City's General Plan was adopted on December 20, 2013, and governs the land uses planned for the SP area. The area is designated Fleming Ranch SP. The land uses designated for undeveloped land near the SP area include 2.1 to 5 dwelling units/acre residential (2.1-5R) and rural residential 5-acre minimum (RR5) to the north and east; 5.1-8 dwelling units/acre residential (5.1-8R) to

the east and south; and public/quasi public facilities (PF) and commercial uses under the Economic Development Corridor (EDC) designation to the south. (See Figure 1.5: Menifee General Plan Map 2017). The General Plan also identifies the northwest area of the SP as a community gateway.

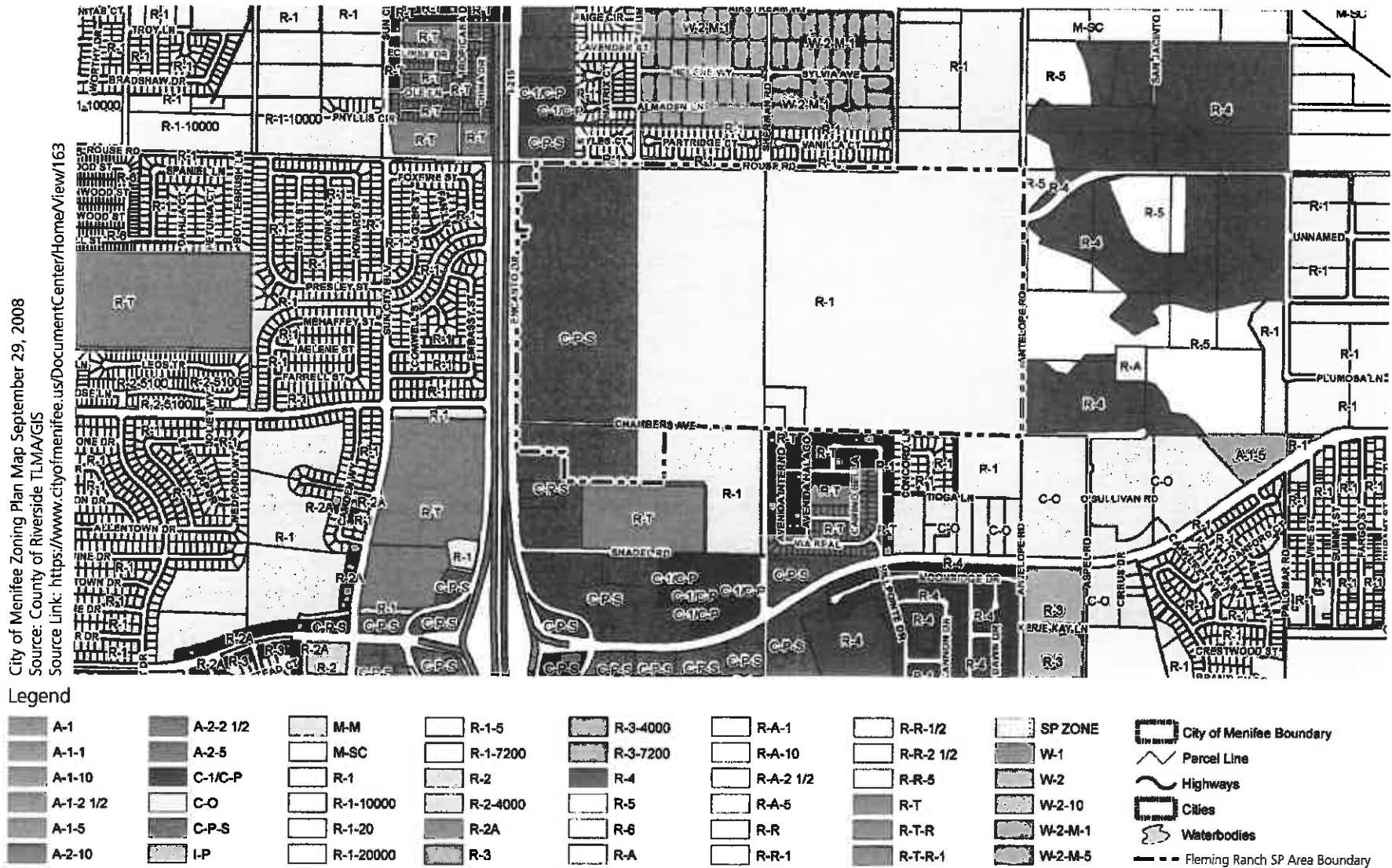


**FIGURE 1.5: MENIFEE GENERAL PLAN MAP 2017**

# FLEMING RANCH SPECIFIC PLAN

As shown in Figure 1.6: Menifee Zoning Map 2017, the existing zoning of the site is predominately One-Family Dwellings (R-1) with the portion adjacent to

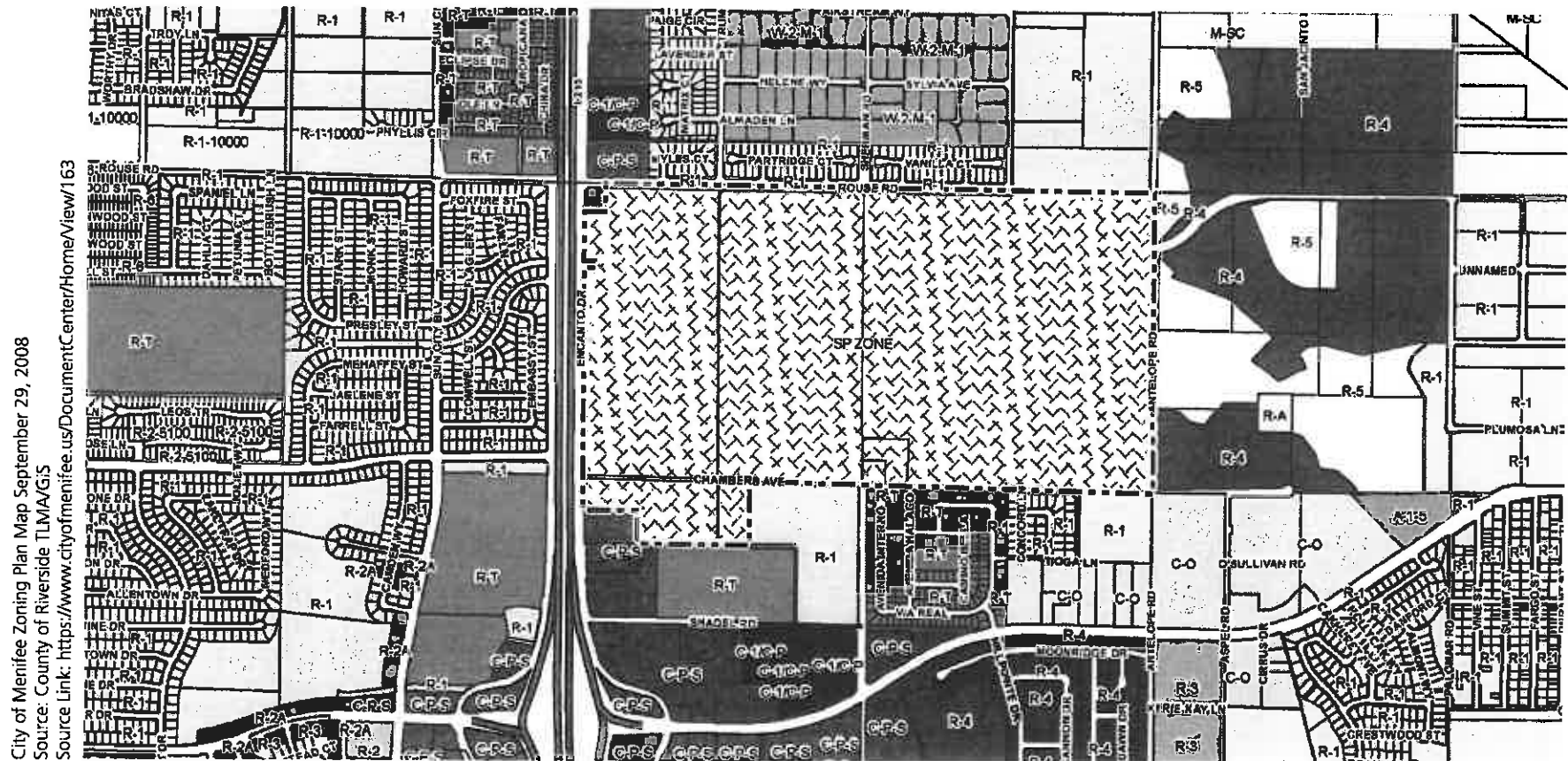
Encanto Drive zoned Scenic Highway Commercial (C-P-S). This SP changes the zoning to SP Zone. (See Figure 1.7: Menifee Zoning Map as Amended).



**FIGURE 1.6: MENIFEE ZONING MAP 2017**

# SECTION 1

## INTRODUCTION



City of Menifee Zoning Plan Map September 29, 2008  
 Source: County of Riverside TLMAGIS  
 Source Link: <https://www.cityofmenifee.us/DocumentCenter/Home/View/163>

### Legend

A-1	A-2-2 1/2	M-M	R-1-5	R-3-4000	R-A-1	R-R-1/2	SP ZONE	City of Menifee Boundary
A-1-1	A-2-5	M-SC	R-1-7200	R-3-7200	R-A-10	R-R-2 1/2	W-1	Parcel Line
A-1-10	C-1/C-P	R-1	R-2	R-4	R-A-2 1/2	R-R-5	W-2	Highways
A-1-2 1/2	C-O	R-1-10000	R-2-4000	R-5	R-A-5	R-T	W-2-10	Cities
A-1-5	C-P-S	R-1-20	R-2A	R-6	R-R	R-T-R	W-2-M-1	Waterbodies
A-2-10	I-P	R-1-20000	R-3	R-A	R-R-1	R-T-R-1	W-2-M-5	Flaming Ranch SP Area Boundary

FIGURE 1.7: MENIFEE ZONING MAP AS AMENDED

## 1.2 OPPORTUNITIES & CONSTRAINTS

The following is a list of opportunities and constraints that were encountered in the land use plan design for the SP. (See Figure 1.8: Opportunities & Constraints Map). For a comprehensive analysis of all environmental factors that were evaluated for this SP, please review the accompanying Environmental Impact Report (EIR) No. 2017-XX.

### 1.2.1 EDGE CONDITIONS

The SP area includes a variety of edge conditions along all four defining boundaries. I-215 parallels the western edge of the SP area with its noise and air quality impacts. Design of the areas adjacent to the freeway acknowledges these constraints by allowing commercial buildings, and well landscaped buffers.

Where on-site development is to occur directly adjacent to previously developed areas, building setbacks with landscaping and fencing provide adequate buffers. Additionally, the design of the roadway system insures adequate access to adjacent developments, while not creating intersection spacing conflicts.

There are two approved TTMs that lie north and east of the SP area. Integration with the design of these maps constrains roadway, trail, and storm drain design. While these constraints are considered minor, they do play a significant part in the design of the overall land use plan for Fleming Ranch.

Infill development is defined by development that is in close proximity to existing infrastructure (transportation corridors, regional drainage, water and sewer lines) and is surrounded by existing and approved development.

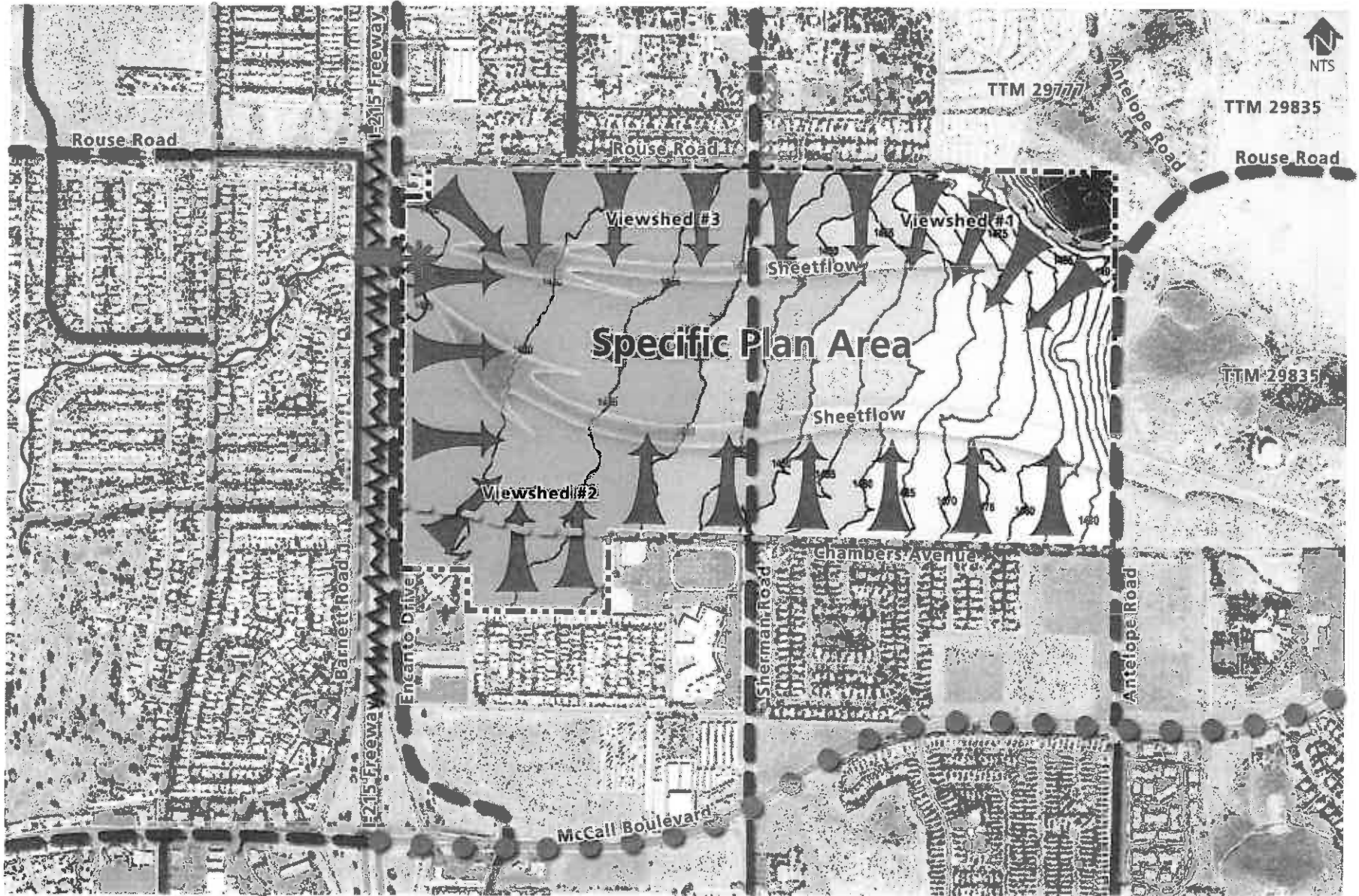
### 1.2.2 VIEWSHEDS

Three major viewshed opportunities exist within the SP area. Viewshed #1, from the northeastern portion of the SP area, provides views of west Menifee and the hillsides that define the edge of the City. Viewshed #2, from the southern and western portions of the SP area, provides views of the on-site knoll, adjacent hills and the distant Lakeview and San Jacinto mountain ranges. Viewshed #3, from the northwestern portion of the SP area, provides views of the adjacent hills to the south.

All of the viewsheds provide view opportunities to on- and off-site knolls and rolling hills that define the uniqueness of the site. It is possible that development of the SP area will impact the current viewsheds; however, the sensitive placement of landscaping and/or buildings within these open spaces can also assist in viewshed preservation.

# SECTION 1

## INTRODUCTION



### Legend

- |                                |                   |                       |                     |
|--------------------------------|-------------------|-----------------------|---------------------|
| Fleming Ranch SP Area Boundary | Secondary Roadway | Channel               | High Point (1,650') |
| Urban Arterial                 | Collector Roadway | Culvert Undercrossing | Low Point (1,425')  |
| Major Roadway                  | Highway Noise     | Viewsheds             |                     |

**FIGURE 1.8: OPPORTUNITIES & CONSTRAINTS MAP** AUGUST 2017 DRAFT 1-15

### **1.2.3 AIRPORT LAND USE COMPATIBILITY PLANNING (ALUCP) FOR MARCH AIR RESERVE BASE**

The ALUCP for March Air Reserve Base was adopted in November 18, 2014 and the Fleming Ranch SP area is located within the Airport Influence Area Zone E but outside noise and safety impact zones. No land use restrictions are placed on the property but due to flight patterns the height of any structure cannot exceed 100 feet without FAA approval. If a structure exceeds 100 feet in height or has the potential to be a hazard to flight due to visual glare or electronic interference the project applicant is required to notify the FAA by electronically filing a 7460-1 Form.

### **1.3 DOCUMENT PURPOSE**

The purpose of the SP is to provide the land use framework for the development of an approximate 331-acre site with a mix of residential, mixed-use, public facility, recreational and open space uses. Specifically, its purpose is to:

- ❖ Determine the appropriate location and intensity of development and mix of land uses within the SP area;
- ❖ Guide the character of land planning to ensure that high-quality improvements are made to create a safe and inviting community;
- ❖ Establish public and private sector implementation measures and responsibilities that adequately address both local and regional needs;
- ❖ Define the future location and dimensions of roadways and other access ways;
- ❖ Identify basic utilities, infrastructure, and public services necessary to support the community; and
- ❖ Institute planning concepts, design guidelines, utility design, and building techniques that are environmentally responsible.

The SP provides the City of Menifee, developers, community groups and service districts with a comprehensive set of plans, regulations, conditions and programs for guiding the systematic development. In addition to approval of this SP, four other accompanying actions were concurrently approved:

- ❖ Change of Zone No. 2017-XX to modify the existing zoning of R-1 (One-Family Dwelling) and C-P-5 (Scenic Highway Commercial) to SP (Specific Plan).
- ❖ TTMs XXXXX and XXXXX to subdivide the property into lots and implement improvements as outlined in this SP.
- ❖ EIR No. 2017-XX to disclose potential environmental impacts resulting from implementation of the SP, in accordance with the CEQA.

The SP has been prepared pursuant to the provisions of California Government Code § 65451, Article 8, authorizing local government agencies the authority to prepare specific plans of development for any area covered by a General Plan, for the purpose of establishing systematic methods of implementation of the agency's General Plan. California Government Code §§ 65450-65456 establish the authority to adopt a SP, identify the required contents of a SP, and mandate consistency with the agency's General Plan. According to § 65451, a SP shall include a text and a diagram or diagrams, which specify the following details:

- ❖ The distribution, location and extent of land uses, including open spaces within the area covered by the Plan.
- ❖ The distribution, location, extent and intensity of major components of the public and private transportation, sewage, water, drainage and other essential facilities located within the area covered by the Plan and are necessary to support the land uses described in the Plan.



# SECTION 1 INTRODUCTION

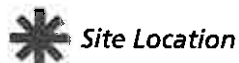
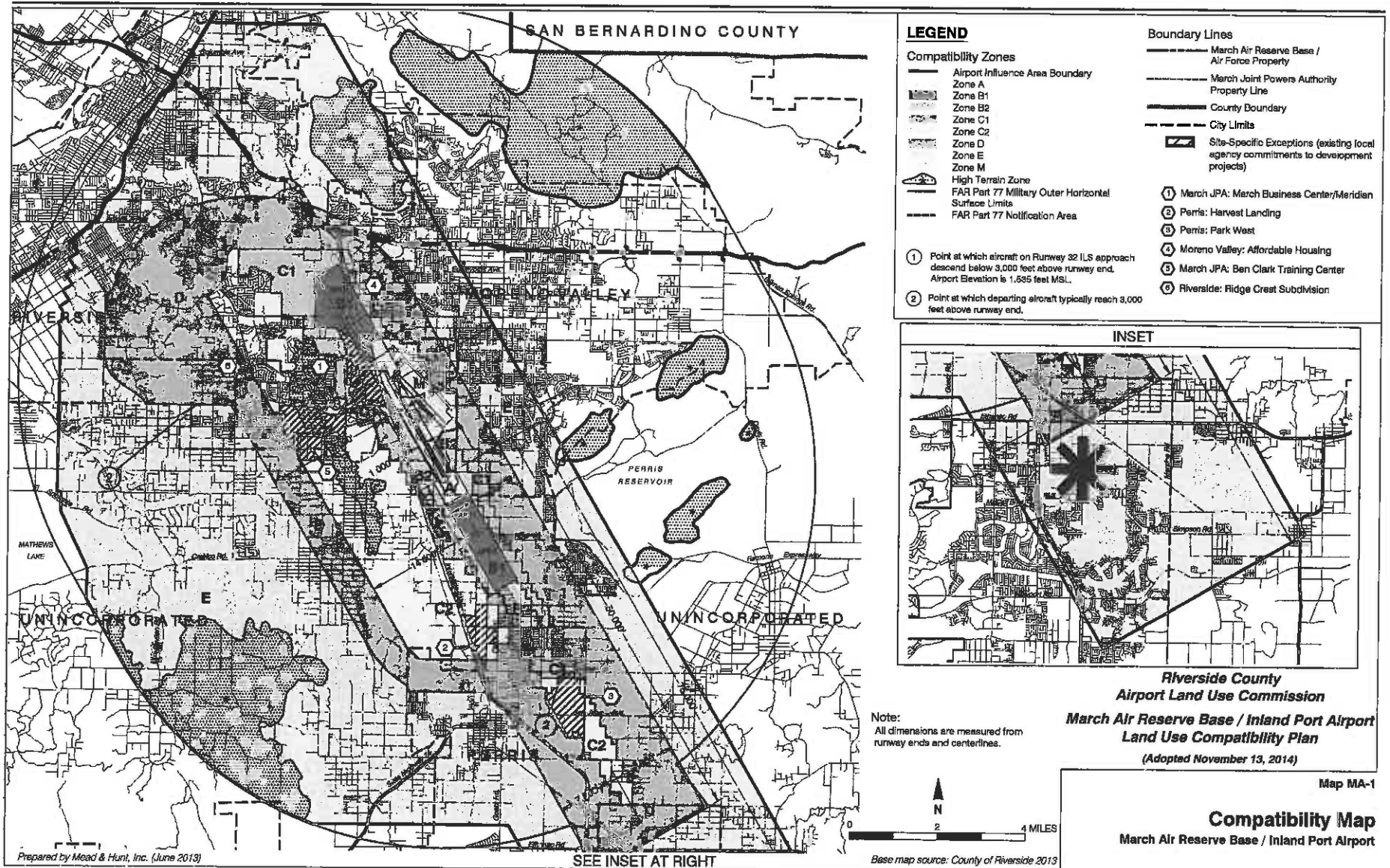


FIGURE 1.9: AIRPORT LAND USE COMPATIBILITY MAP

- ❖ The standards and criteria by which development will proceed, and standards for the conservation, development and utilization of natural resources, where applicable.
- ❖ A program of implementation measures including regulations, programs, public works projects and financing measures necessary to carry out the items listed above.

In response to government requirements, this SP has been prepared to provide the essential link between the policies and objectives of the General Plan and the development plan for the Fleming Ranch property. By functioning as a regulatory document, this SP implements the General Plan within the boundaries of the SP area. In this regard, all future development plans and entitlements for the Fleming Ranch property shall be consistent with the regulations set forth in this document and applicable City regulations. This SP identifies site-specific design requirements applicable within the Fleming Ranch property and, as such, adherence to this SP will ensure that new development meets or exceeds City standards for environmental safety, infrastructure and site planning while providing provisions for maintenance, aesthetic quality and community identity.

# SECTION 2



---

## COMMUNITY DEVELOPMENT PLAN

---

This Specific Plan features a traditional neighborhood lifestyle with a variety of housing types that are within easy walking distance to recreational amenities. Pedestrian connectivity is provided through a system of pedestrian trails, sidewalks and bicycle lanes that link residential neighborhoods to one another, to open space, and to recreational amenities.

## 2.1 THE VISION

The Land Use Plan for Fleming Ranch evolved from the following basic objectives:







- ❖ Design a Plan that blends seamlessly into the fabric of the existing community and development that surrounds the site.
- ❖ Establish two residential villages with recreational opportunities and nearby places to work or shop.
- ❖ Provide a mix of residential lot sizes to serve the needs of future residents.
- ❖ Allow the potential for a private, gated and age-qualified (55 and older) Village within the community.
- ❖ Incorporate amenities, open spaces, trails and public facilities throughout the entire Plan.
- ❖ Implement and utilize sustainable principles during all phases of project design, implementation and construction.

Within the community, residents will be able to utilize an integrated system of pedestrian trails, sidewalks and bike lanes to access recreational amenities. Streets within are planned to function as a “promenade” and will feature lush community-based landscaping, helping define the sense of arrival. Meandering sidewalks and trails throughout the community will connect neighborhoods.

## 2.2 PROPOSED LAND USES

Table 2.1: Land Use Summary provides a summary of the proposed land uses shown on Figure 2.1 Land Use Plan.

**TABLE 2.1: LAND USE SUMMARY**

LAND USE	GROSS AREA (ACRES)	DENSITY RANGE (DU/AC)	MAXIMUM INTENSITY (SF)	MAXIMUM DWELLING UNITS
 Commercial	20.4	-	225,000**	-
 Medium Density Residential (MDR)	222.5*	3.3-5.7		1,080
 Open Space Recreation (OS-R)	17.7	-	-	-
 Open Space Conservation (OS-C)	6.3	-	-	-
 Water Quality/Detention Basins(OS-W)	27.5	-	-	-
 Roadways	36.6	-	-	-
<b>Total</b>	<b>331.0</b>	<b>3.3-5.7</b>	<b>225,000**</b>	<b>1,080</b>
* A 68.3-acre Age-Qualified Overlay allowing 310 dwelling units is allowed on Planning Areas 8-11				
** Retail square footage may be converted to 275,000 SF of office or 350,000 SF of other non-residential uses				

# SECTION 2

## COMMUNITY DEVELOPMENT PLAN



**FIGURE 2.1 LAND USE PLAN**

As shown on Figure 2.2: Conceptual Development Plan, the SP can be designed with smaller neighborhood units to reinforce social interaction among residents and to facilitate aesthetic differentiations between neighborhoods. The Plan is also designed with a local street network of short blocks and cul-de-sacs that allow very little through-traffic.

### 2.2.1 COMMERCIAL

Two parcels within the Specific Plan are designated as Commercial similar to the Economic Development Corridor land uses north and south along Encanto Drive facing the I-215 freeway. Up to 225,000 square feet of commercial development is permitted. However, in an effort to maintain flexibility to respond to changing community needs and market conditions over the build-out Fleming Ranch, commercial square footage could be converted to either office or other non-residential uses provided that trip generation is less than commercial. A parking lot for an off-site use (the adjacent mortuary) is allowed. A 20-foot landscape buffer is required adjacent to residential use.

**TABLE 2.2: COMMERCIAL PLANNING AREA SUMMARY**

PLANNING AREA	ACREAGE	MAXIMUM INTENSITY
Planning Area 16	17.8	200,000
Planning Area 17	2.6	25,000
<b>Total</b>	<b>20.4bn</b>	<b>225,000**</b>

\*\* Retail square footage may be converted to 275,000 SF of office or 350,000 SF of other non-residential uses
















### 2.2.2 RESIDENTIAL LAND USES

Medium Density Residential (MDR) land uses account for 67% of the entire SP area. A total of 1,080 homes are allowed an average density of 4.9 du/ac. The MDR land use classification includes conventional single-family detached homes on minimum lot sizes varying between 5,000 and 7,000 square feet. Front yard setbacks vary along the street scene. Private yard space is concentrated on the side and rear of the home.

Adjacent to Chambers Avenue and Rouse, Sherman and Antelope Roads, a 10-foot landscape easement will be provided adjacent to the paseos creating additional landscape and separation between the roadways and the residential lots.

Though the conceptual development plan is illustrative in nature and the final placement of lots will be determined during the tentative tract map process, preliminary lotting studies indicate that 9 to 16% of the lot sizes have an average lot size up to 20% larger than the minimum lot size standards.

**TABLE 2.3: RESIDENTIAL PLANNING AREA SUMMARY**

	LAND USE	ACREAGE	DENSITY	DUs
<b>West Village</b>				
	PA1 5,000 S.F. Lots	13.8	5.4	74
	PA2 6,000 S.F. Lots	10.7	4.6	49
	PA3 5,500 S.F. Lots	26.6	5.1	135
	PA4 6,500 S.F. Lots	11.5	4.1	47
	PA5 6,500 S.F. Lots	4.2	4.5	19
	PA6 5,000 S.F. Lots	17.8	5.6	100
	PA7 6,000 S.F. Lots	17.2	4.7	81
<b>Subtotal West Village</b>		<b>101.8</b>	<b>5.0</b>	<b>505</b>
<b>East Village</b>				
	PA8 5,000 S.F. Lots	16.2	5.4	88
	PA9 6,500 S.F. Lots	12.7	3.3	42
	PA10 5,500 S.F. Lots	12.8	5.0	64
	PA11 5,500 S.F. Lots	26.6	4.7	124
	PA12 7,000 S.F. Lots	12.2	4.0	49
	PA13 5,000 S.F. Lots	14.4	5.7	82
	PA14 6,500 S.F. Lots	12.1	4.5	54
	PA15 5,500 S.F. Lots	13.7	5.3	72
<b>Subtotal East Village</b>		<b>120.7</b>	<b>4.8</b>	<b>575</b>
<b>Total</b>		<b>222.5</b>	<b>4.9</b>	<b>1,080</b>

**SECTION 2**  
**COMMUNITY DEVELOPMENT PLAN**

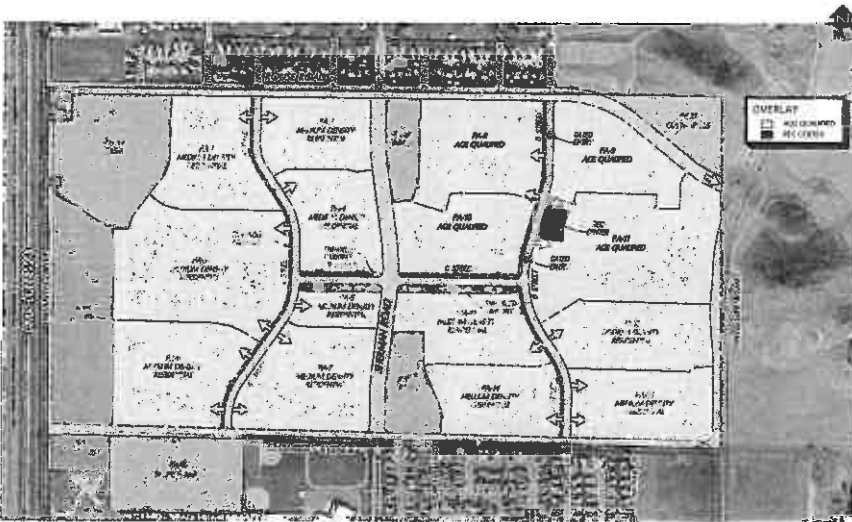


**FIGURE 2.2: CONCEPTUAL DEVELOPMENT PLAN**

The Conceptual Development Plan illustrates a land use combination that could be implemented under the provisions of this SP. As such, the Conceptual Development Plan is illustrative in nature and the final alignments of streets and the placement of lots will be determined during the tentative tract map process. Chapter 4.0, Development Standards contains specific development standards and zoning criteria that would apply.

### 2.2.3 AGE-QUALIFIED OVERLAY

An Age Qualified Overlay is permitted in the northwest portion of the East Village. This Overlay reduces the number of homes allowed because a private one-acre recreation facility is included for exclusive use by the age-qualified residents. Refer to Figure 2.3: Age-Qualified Overlay.



**FIGURE 2.3: AGE-QUALIFIED OVERLAY**

### 2.2.4 OPEN SPACE AND RECREATIONAL LAND USES

An important element of the SP is the provision of recreation and open spaces to enhance the quality of living for residents of the community. As illustrated in Figure 2.4: Park and Open Space Conservation Diagram and Figure 2.5: Basin Diagram, the SP includes a network of parks, natural open space areas, enhanced landscape areas,

and water quality and detention basins. Individual components of the open space system are discussed and graphically depicted on the following pages. Conceptual plans of the open space and recreation areas are provided to present initial designs that could be included in future design proposals. Overall, the Plan provides for open space and recreational uses, supplemented by a series of smaller activity nodes. Table 2.4: Open Space & Recreation Planning Area Summary summarizes each of the open space and recreational elements within the SP and the subsections that follow provides a more thorough discussion of each.

**TABLE 2.4: OPEN SPACE & RECREATION PLANNING AREA SUMMARY**

SPECIFIC USE	ACRES
Sports Park (OS-R)	12.9
Enhanced Paseos (OS-R)	4.8
Open Space Conservation (OS-C)	6.3
<b>Subtotal Parks and Open Space</b>	<b>24.0</b>
Water Quality and Detention Basins (OS-W)	27.5
<b>Grand Total</b>	<b>51.5</b>

### A. PARKS

As shown in Figure 2.4: Park and Open Space Conservation Diagram, a major component of the SP is the Sports Park and the enhanced paseos with activity nodes and multi-use trails that are strategically distributed throughout the site. As conceptually shown, every home would be located within 1/4 mile of a trail.

The multi-purpose Sports Park allows for lighted sports fields and picnic areas to meet the needs of the region, as well as to the future residents of the SP. The Sports Park will be accessible from Chambers Avenue and will be open to the public.

The enhanced paseos provide passive and active recreational uses (walking, biking, tot lots and amenities) and an open space buffer between collector streets and neighborhoods. The enhanced paseos include eight-foot wide multi-purpose trails



providing non-vehicular access (with the exception of maintenance vehicles). The width of these 20- to 60-foot enhanced paseos provide a landscaping opportunity. Activity nodes are planned providing active recreational amenities that may consist of adult exercise stations, tot lots or other similar recreational elements. The enhanced paseos do not include right-of-way area in the area calculations.

**B. QUIMBY REQUIREMENTS**

California Government Code §66477 (passed as a part of the 1975 Quimby Act) requires cities and counties to pass ordinances requiring that developers set aside land, donate conservation easements and/or pay fees for park improvements.

The City of Menifee has set a goal of providing 5 acres of active parkland for every 1,000 residents. Menifee Municipal Code (MMC) §9.55.060 outlines both the required parkland dedication and the average number of persons per residential development.

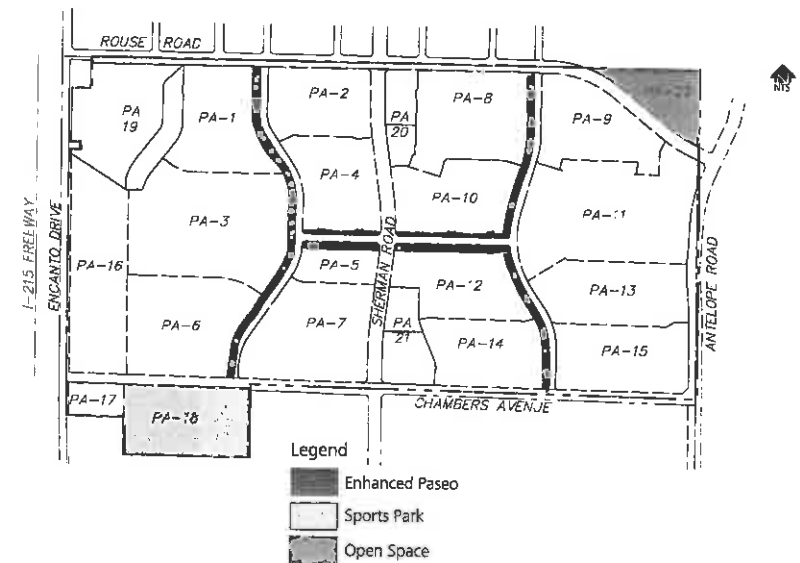
The following table highlights the SP’s consistency with the City of Menifee requirements.

**TABLE 2.5: PARK CALCULATIONS**

REQUIRED PARK CALCULATIONS		
Up to 1,080 homes	3.164 persons per household	3,417 persons (population)
	Divide by 1,000	3.41
	Required parkland acreage ( X 5)	17.1
	Active park acres planned	17.7
	Over/Under a Minimum of	+ 0.6 acres

**C. OPEN SPACE CONSERVATION**

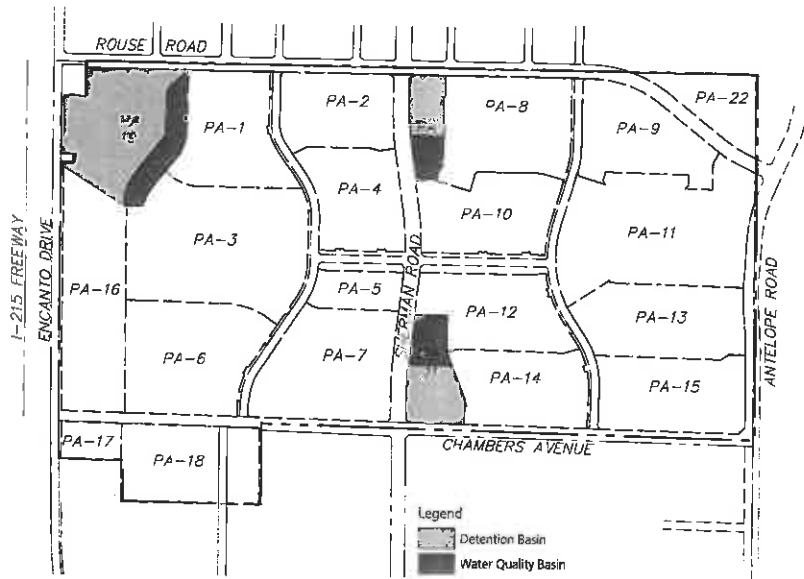
The Open Space - Conservation (OS-C) land use designation provides for the preservation of a landmark knoll with the highest elevation on-site. This open space area consists of grassland, rock outcroppings and includes dirt trails. Adjacent approved tract maps also provide for the preservation of open space surrounding this space that will ultimately result in a larger, continuous conservation area. This area of open space conservation is not within the Western Riverside County Multiple Species Habitat Conservation Plan (MSHCP) area. It’s important to note that while the land use category is Open Space Conservation, the designation is not intended to imply that this area serves as a habitat conservation area. Rather, for the purposes of this SP, the land is not counted towards developable area and it will remain in its natural habitat.



**FIGURE 2.4: PARK AND OPEN SPACE CONSERVATION DIAGRAM**

**D. WATER QUALITY AND DETENTION BASINS**

The Open Space - Water (OS-W) Land Use designation provides for three water quality and detention basins. The basins will serve as an open space amenity for the community. A conceptual basin concept is depicted in Figure 2.5: Basin Diagram. The basins are envisioned to contain a special landscape treatment that will reinforce the community landscape theme and serve as an open space amenity.



**FIGURE 2.5: BASIN DIAGRAM**

**2.3 CIRCULATION PLAN**

The Circulation Plan reinforces the concept of traditional neighborhood design and proposes a circulation system comprised of roadways, pedestrian pathways and trails to provide for efficient and effective access to and through the site. The Circulation Plan is designed to provide optimal circulation efficiency and safety for guests and residents.

**2.3.1 ROADWAYS**

At the time of Specific Plan approval, the roadways surrounding the Fleming Ranch Specific Plan were in various stages of improvement from fully paved rights-of-way to dirt roads. Figure 2.6: Vehicular Circulation Diagram highlights the primary vehicular routes available to serve the SP area. The General Plan Roadways also provide significant opportunities to enhance vehicular access and traffic flow in and around the SP area.

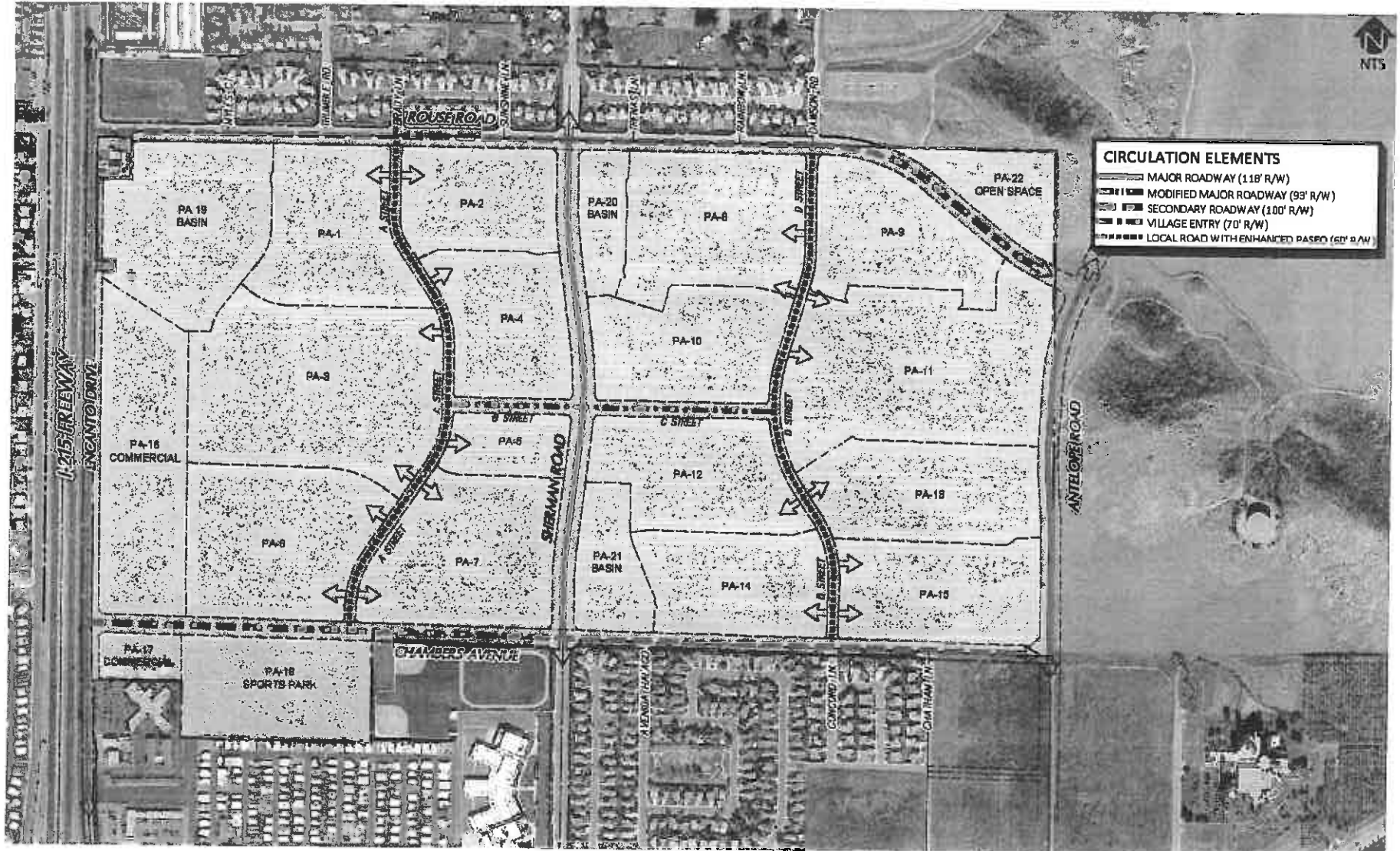
Five General Plan Roadways are designated adjacent to within the SP area:

- ❖ Encanto Drive - Modified Major Roadway (Freeway Frontage Road),
- ❖ Sherman Road - Major Roadway,
- ❖ Antelope Road - Major Roadway,
- ❖ Chambers Avenue - Secondary Roadway, and
- ❖ Rouse Road - Secondary Roadway.

A brief description of each of these roadways is listed on the next pages.

# SECTION 2

## COMMUNITY DEVELOPMENT PLAN



**FIGURE 2.6: VEHICULAR CIRCULATION DIAGRAM**

# FLEMING RANCH SPECIFIC PLAN

## A. ENCANTO DRIVE

Encanto Drive is designated as a Major Roadway on the General Plan. A modification of the ultimate right-of-way is proposed to be 93 feet, four travel lanes (two in each direction) and a striped center median. Encanto Drive is a local frontage road (fronting the eastern right-of-way of I-215) that links to McCall Boulevard (1/2 mile south of the SP area) and terminates in a planned roundabout in a planned commercial center north of McLaughlin Road (3/4 mile north of the SP area). Encanto Drive provides primary access to the commercial areas of the SP.

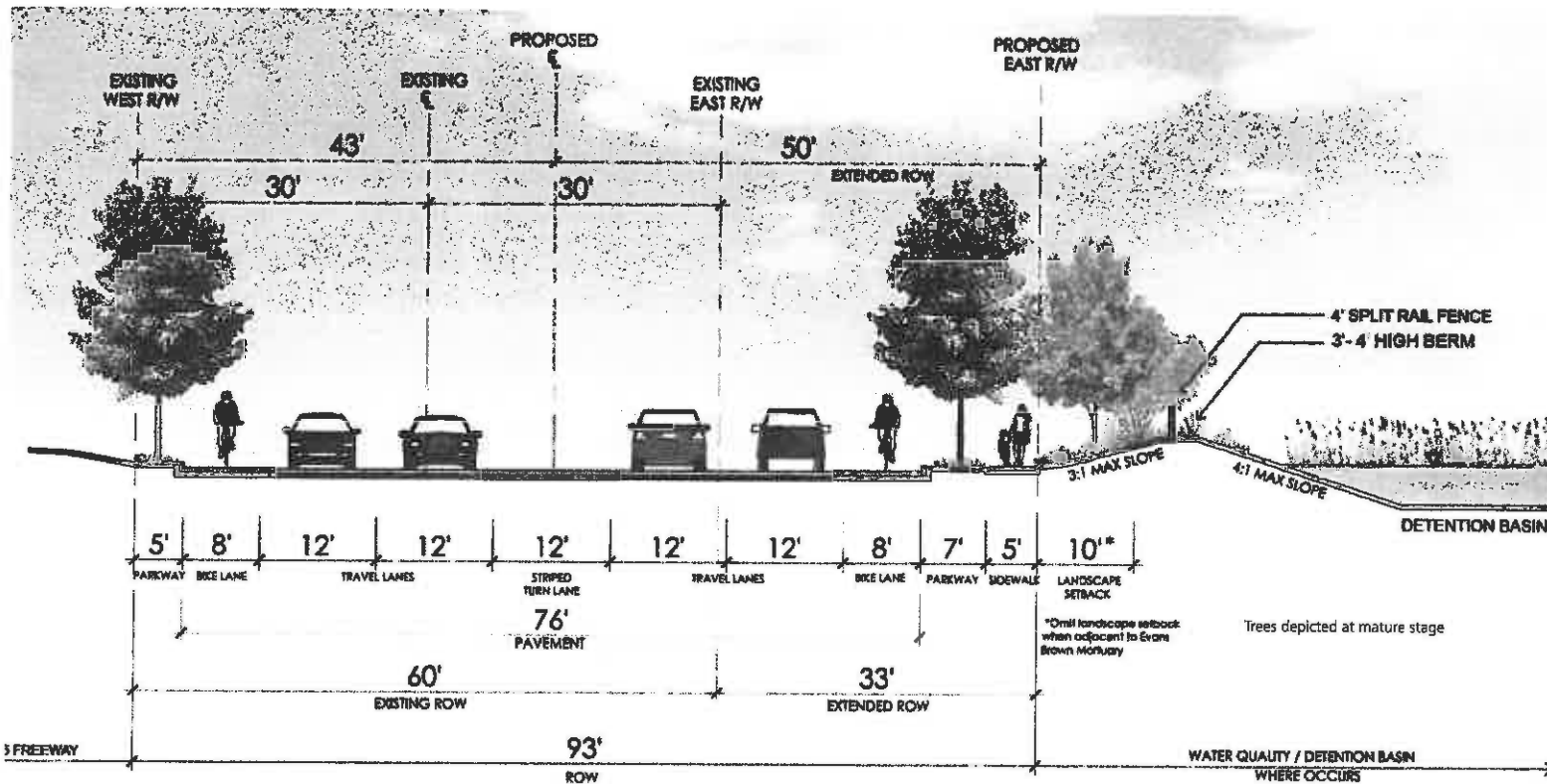
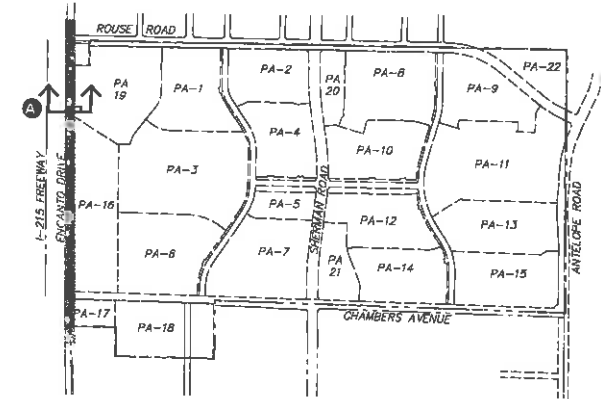
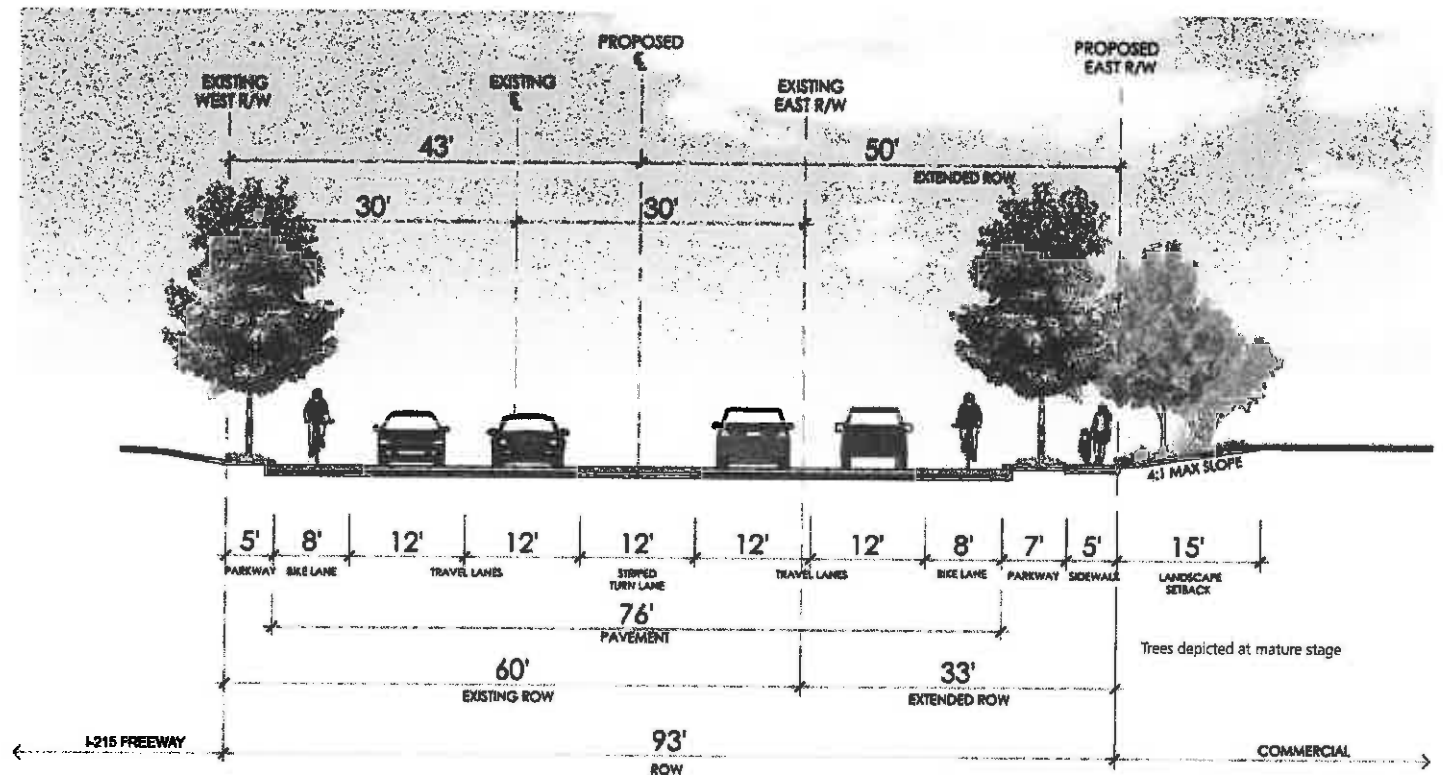
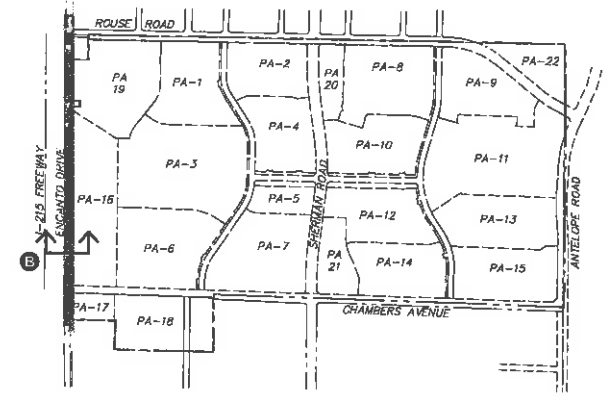


FIGURE 2.7A: ENCANTO DRIVE

## SECTION 2

### COMMUNITY DEVELOPMENT PLAN

An additional 33 feet of right-of-way will be added to the existing condition. Only the eastern right-of-way of Encanto Drive is planned to include a landscaped area with a seven-foot wide landscape parkway and a five-foot sidewalk. The eastern side of the roadway also includes an eight-foot wide on-street class II bike lane. The western side of the roadway (adjacent to I-215) includes a five-foot wide landscape area with no sidewalk and a eight-foot wide on-street class II bike lane. Adjacent to the right-of-way will be a 15-foot wide landscaped setback on PA-16 and PA-17.

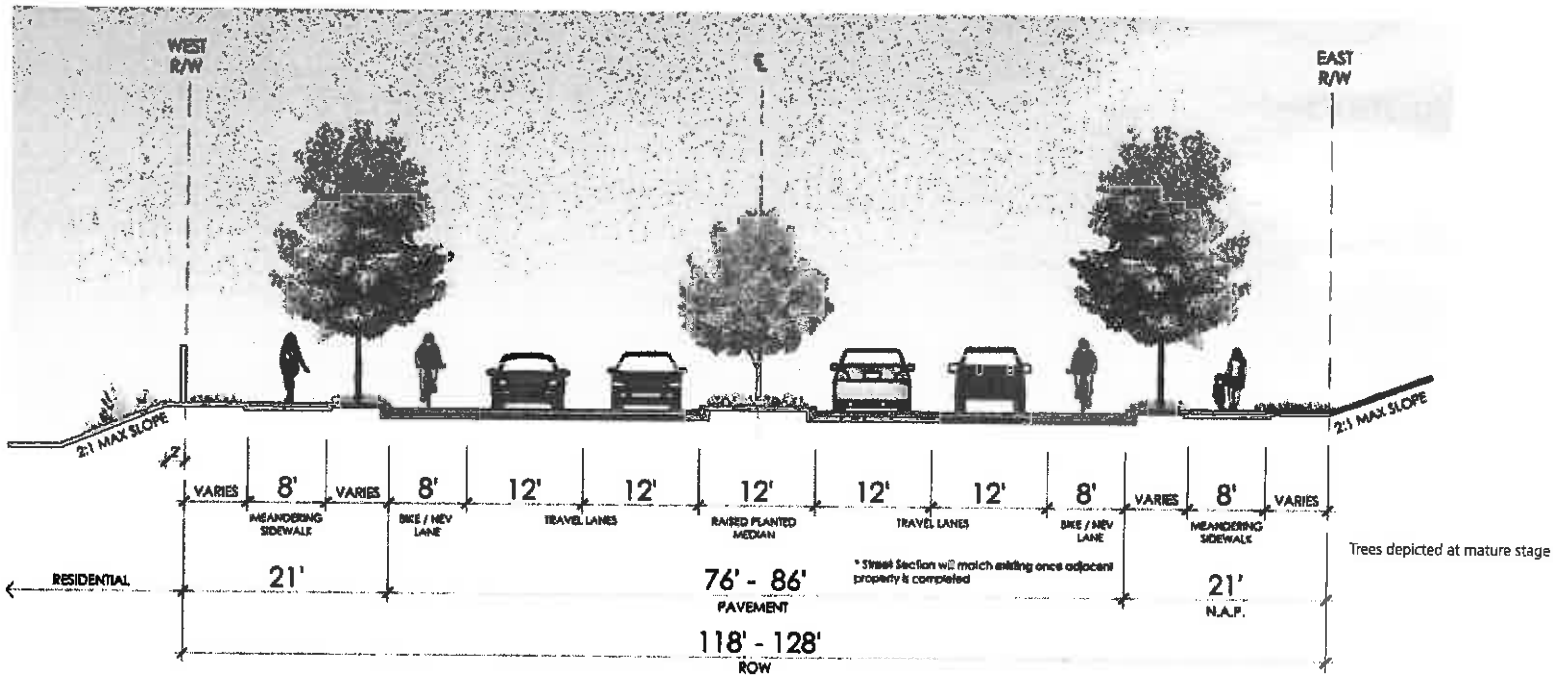
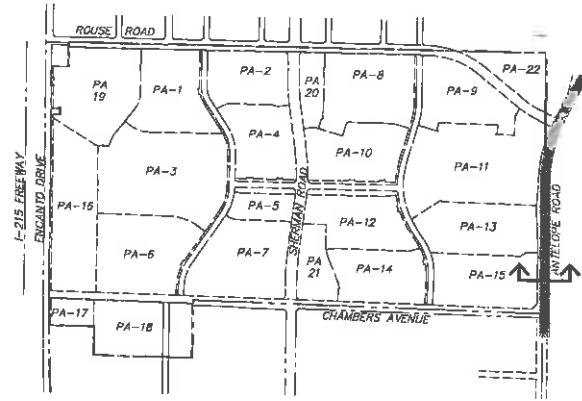


**FIGURE 2.7B: ENCANTO DRIVE**

**B. ANTELOPE ROAD**

Antelope Road is designated as a Major Roadway with an ultimate right-of-way of 118 to 128 feet with four travel lanes (two in each direction) and a center median to match adjacent planned improvements. The segment of Antelope Road that defines the easterly boundary of the SP area begins at McCall Boulevard (1/4 mile south of the SP area) and terminates at the intersection of Rouse Road. In addition, Antelope Road is planned to continue north as a Secondary Roadway, ultimately intersecting with Ethanac Road (one mile north of the SP area). Antelope Road provides secondary access to the SP area.

Antelope Road also includes an eight-foot wide NEV/Class II Bicycle lane on both sides of the street. The right-of-way also includes a meandering eight-foot wide trail within a 21-foot wide landscape area. The east side of the Antelope Road will be improved by others. Figure 2.8: Antelope Road is consistent with current planning efforts on the adjacent parcel.

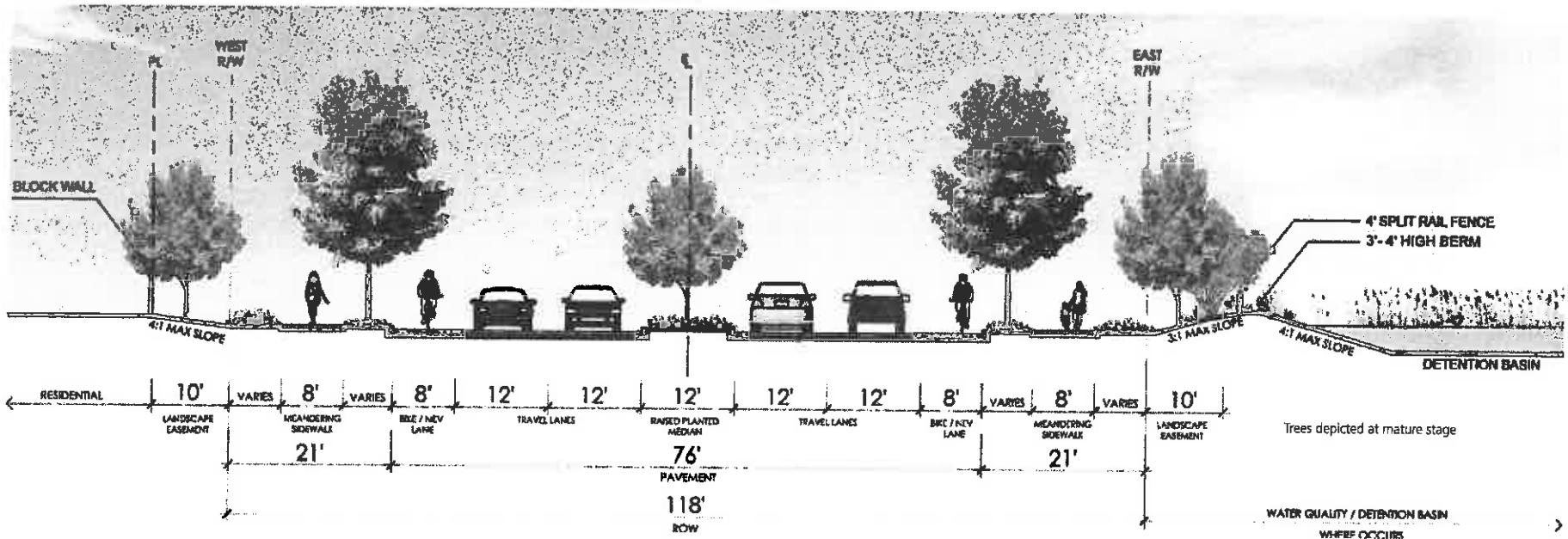
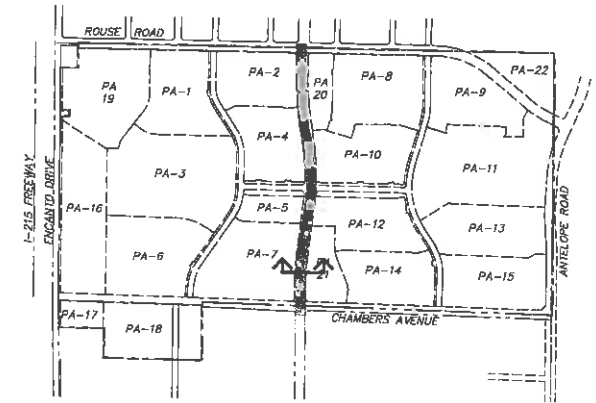


**FIGURE 2.8: ANTELOPE ROAD**

**C. SHERMAN ROAD**

Sherman Road is designated as a Major Roadway with an ultimate right-of-way of 118 feet with four travel lanes (two in each direction) and a center raised planted median. Sherman Road is planned to originate at McCall Boulevard (1/2 mile south of the SP area) and ultimately connect to Watson Road and SR-74 (approximately 1.5 miles north of the SP area). Sherman Road provides the primary vehicular access to the SP area.

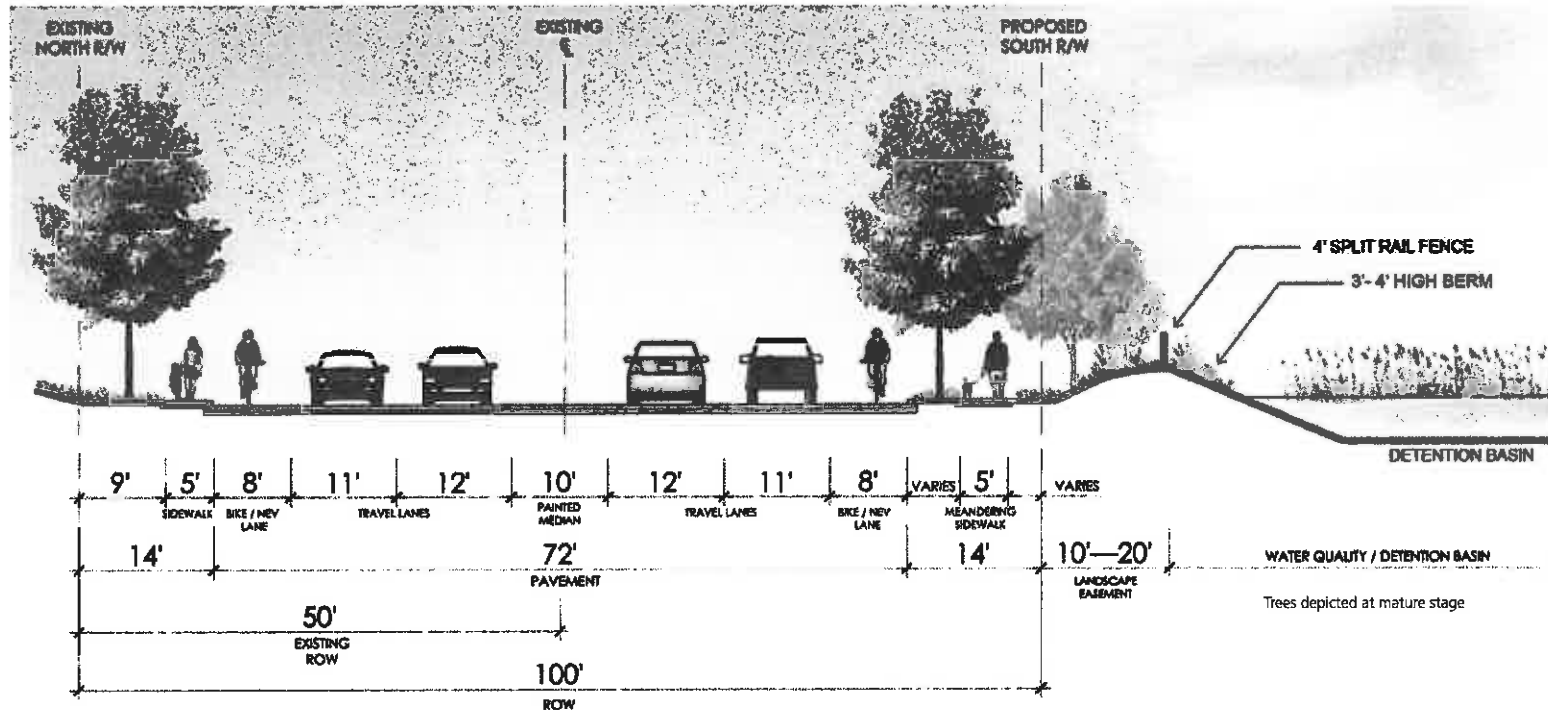
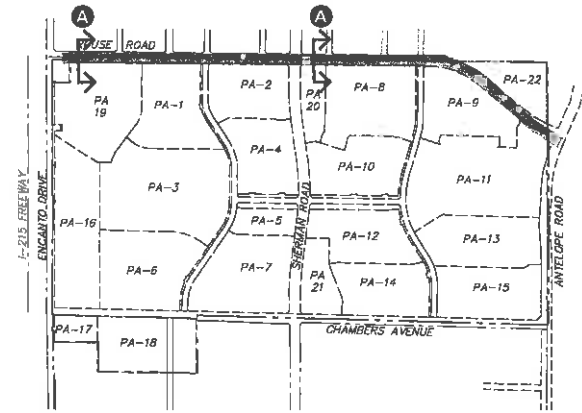
The right-of-way includes a meandering eight-foot wide trail within a 21-foot landscape area with an additional 10-foot wide landscape easement for a total of 23 feet of landscape areas on both sides of Sherman Road. The meandering trail will connect to the curb adjacent sidewalks at the north and south end of the SP area. Sherman Road has been identified as a location for potential future transit service. Future bus turnouts may be accommodated in the proposed parkway.



**FIGURE 2.9: SHERMAN ROAD**

**D. ROUSE ROAD**

Rouse Road is partially improved and is designated as a Secondary Roadway with an ultimate right-of-way of 100 feet with four travel lanes (two in each direction) and a painted median. The segment of Rouse Road that forms the northerly boundary of the SP area originates at Encanto Drive to the west and terminates at a planned intersection with Antelope Road. According to the General Plan, Rouse Road is planned to continue eastward and ultimately connect with Menifee Road (one mile east of SP area). In addition, a separate segment of Rouse Road exists west of I-215. Adjacent to the detention basins, additional landscape will be provided on a three- to four-foot high berm next to the basin.



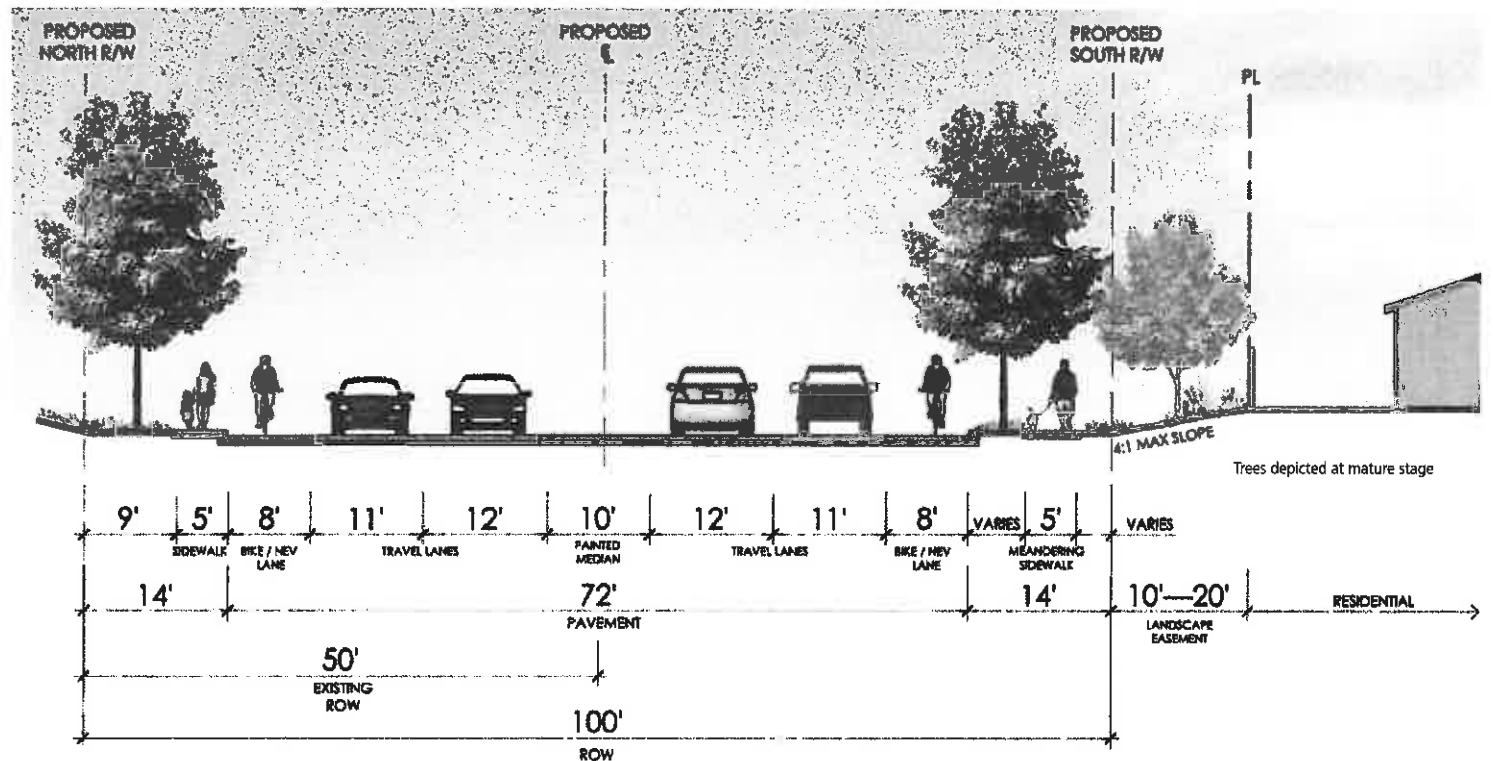
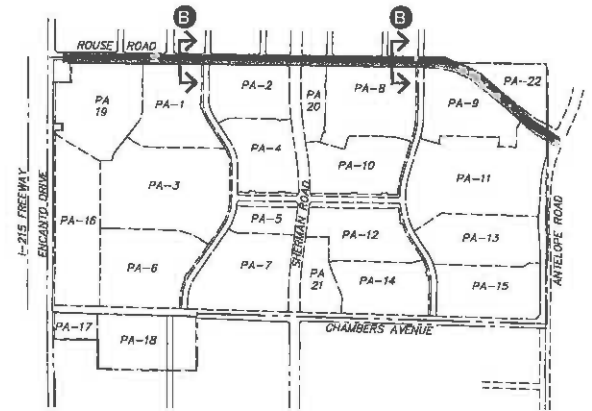
**FIGURE 2.10A: ROUSE ROAD**



# SECTION 2

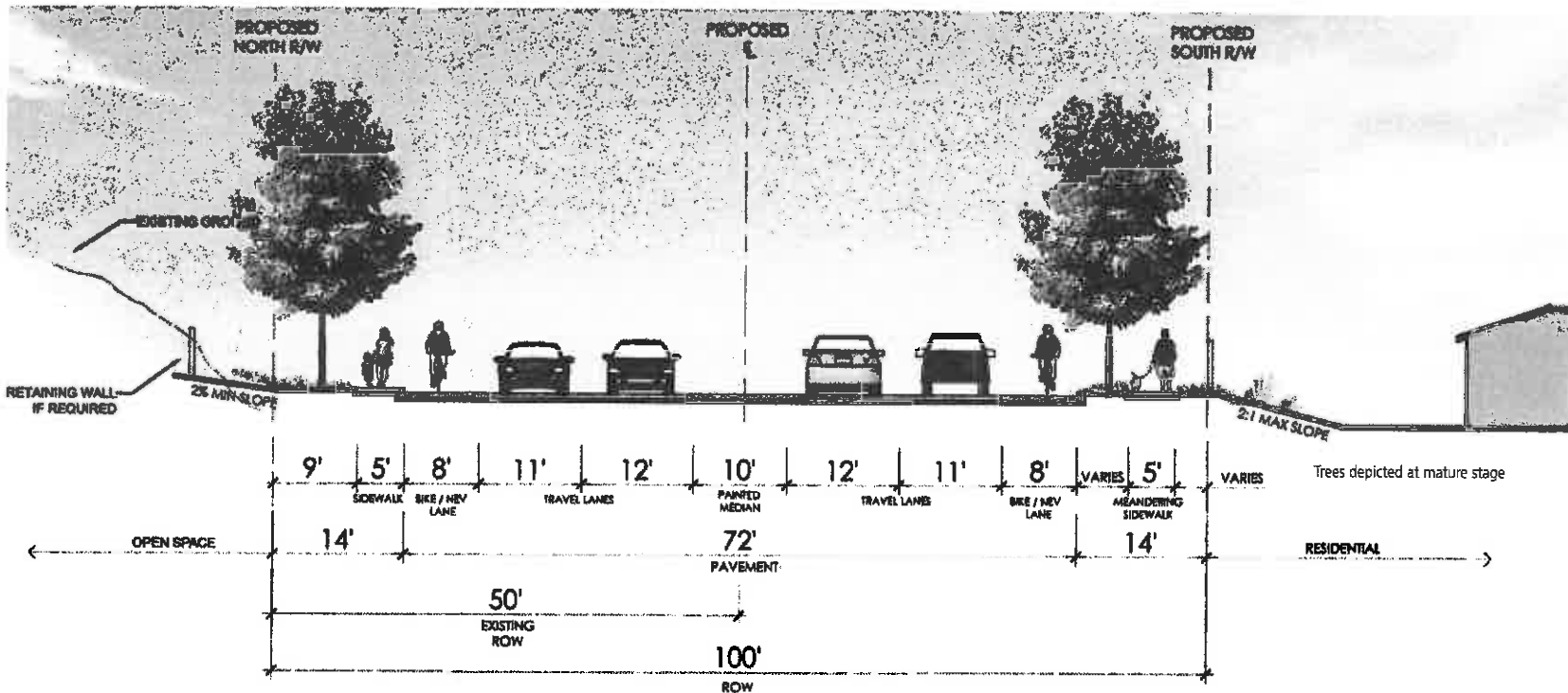
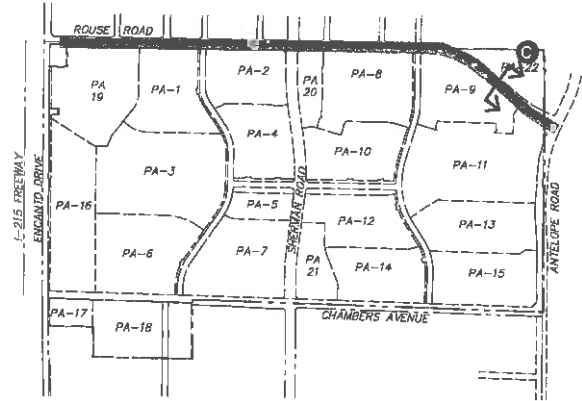
## COMMUNITY DEVELOPMENT PLAN

Rouse Road also includes an eight-foot wide NEV/Class II Bicycle lane on both sides of the street. The right-of-way on the south side adjacent to the SP area includes a meandering five-foot wide trail within a 14-foot landscape area with an additional 10- to 20-foot wide landscape easement for a total of 19 to 29 feet of landscape areas.



**FIGURE 2.10B: ROUSE ROAD**

On the north side of Rouse Road there is an existing curb adjacent sidewalk within a fourteen-foot wide parkway. This curb-adjacent sidewalk will continue on the north side adjacent to PA-22.



**FIGURE 2.10C: ROUSE ROAD**

## SECTION 2

### COMMUNITY DEVELOPMENT PLAN

#### E. CHAMBERS AVENUE

Chambers Avenue is designated as a Secondary Roadway with an ultimate right-of-way of 100 feet with four travel lanes (two in each direction) and a painted center median. Chambers Avenue makes up a portion of the southerly boundary of the SP area and is planned to connect to Encanto Drive on the west and Antelope Road on the east. However, east of Sherman Road the existing condition was not improved to this standard. The northern side of Chambers Avenue will be improved to City standards; therefore the right-of-way will be 94 feet wide. Twenty additional feet of right-of-way will be added with a ten-foot landscape easement.

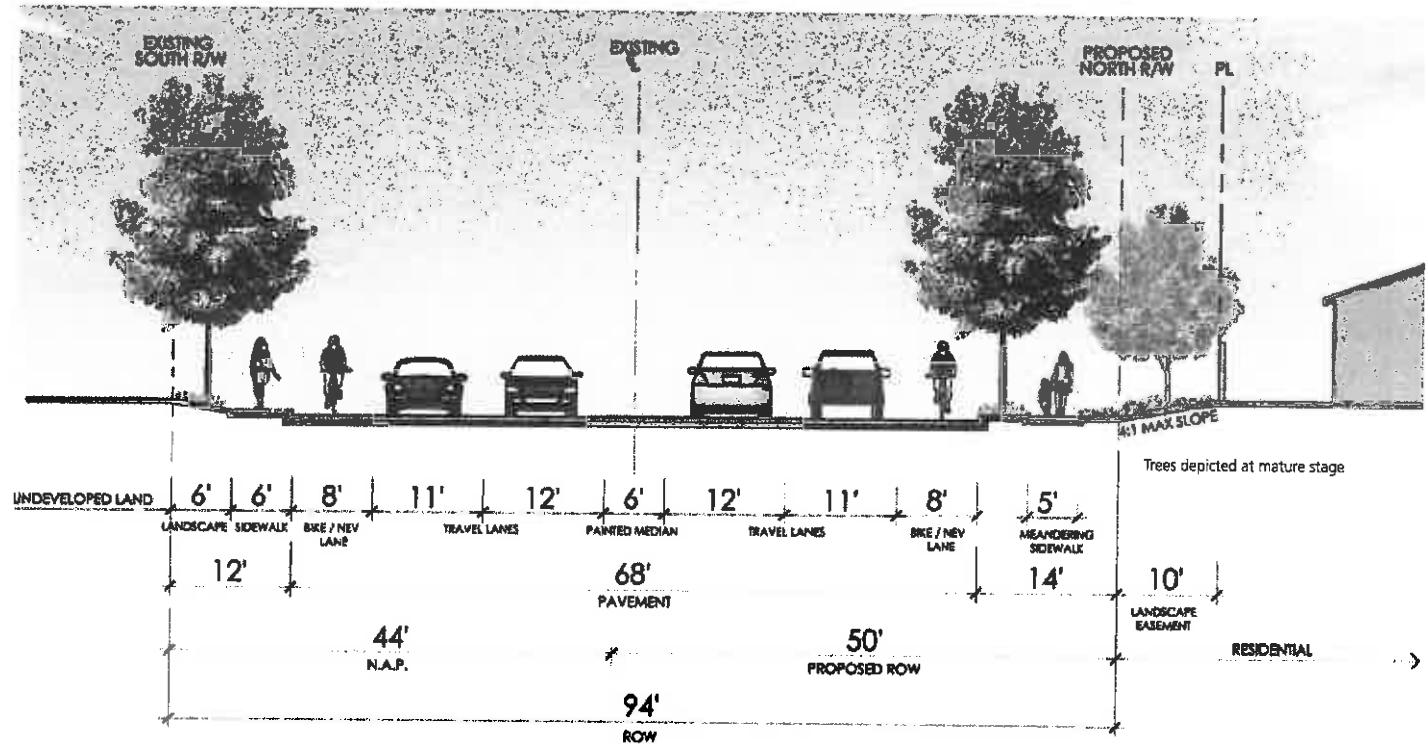
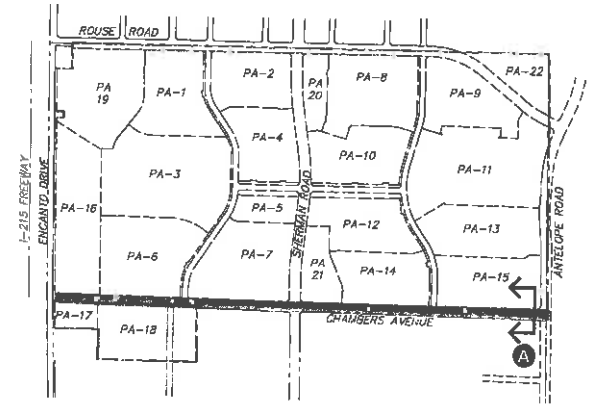


FIGURE 2.11A: CHAMBERS AVENUE

# FLEMING RANCH SPECIFIC PLAN

Chambers Avenue includes an eight-foot wide NEV/Class II Bicycle lane on both sides of the street. The right-of-way on the north side adjacent to the SP area includes a meandering five-foot wide trail within a 14-foot landscape area with an additional 10-foot wide landscape easement for a total of 19 feet of landscape.

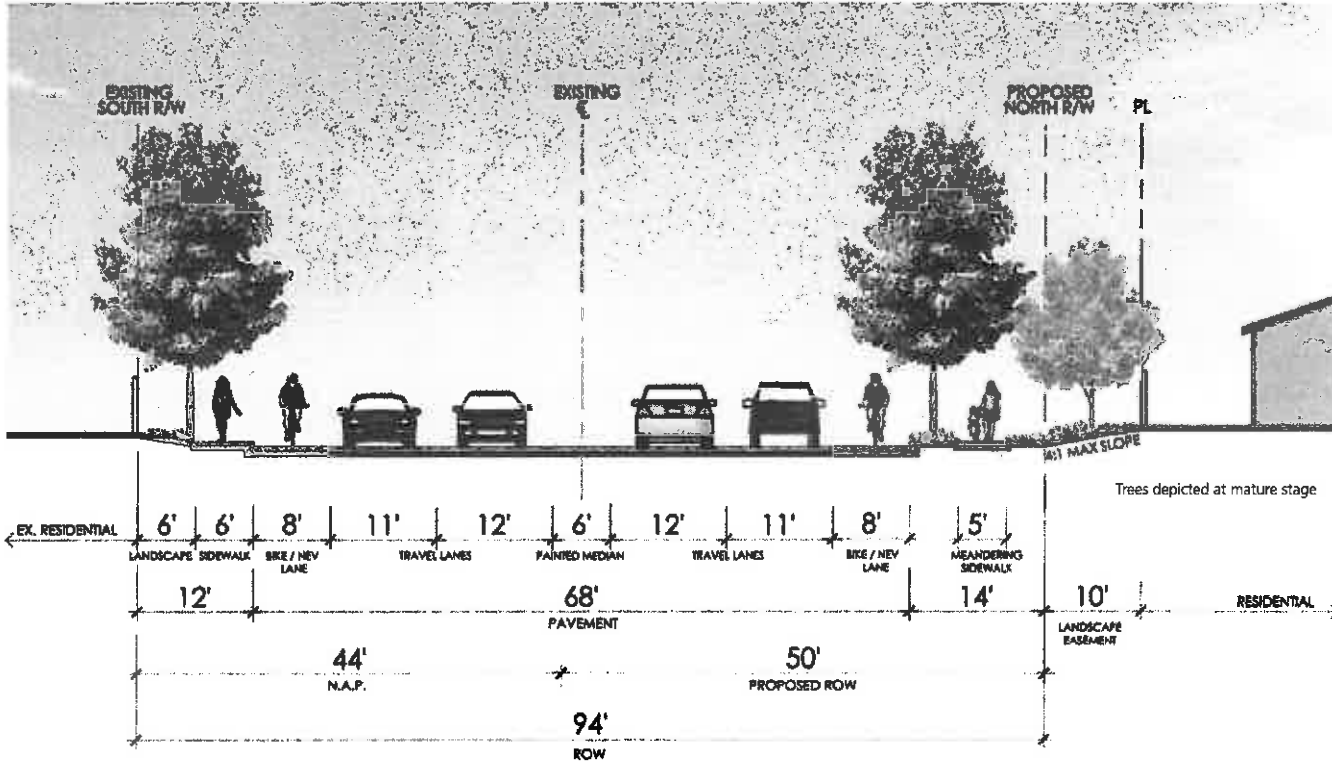
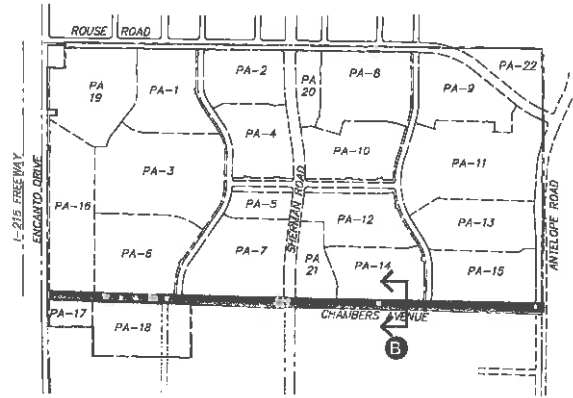
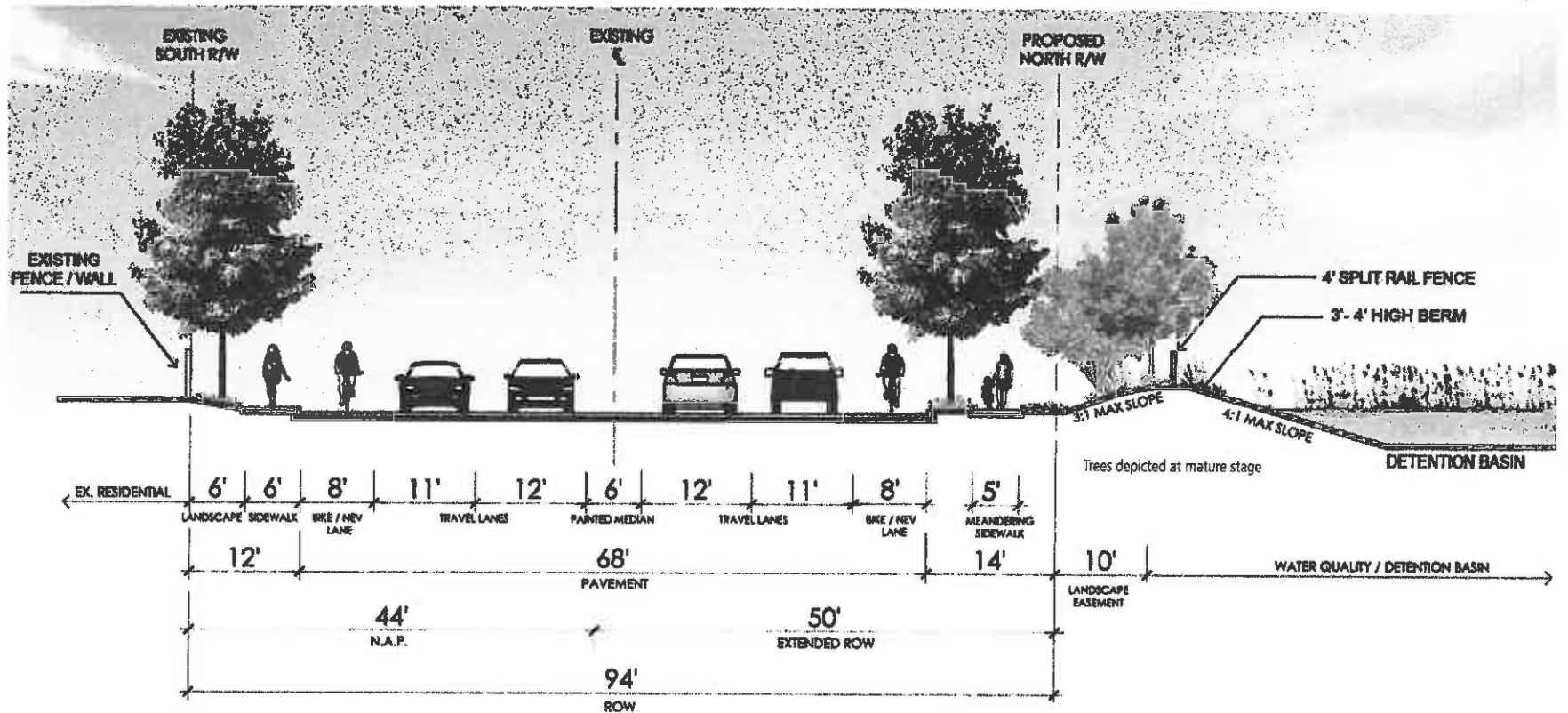
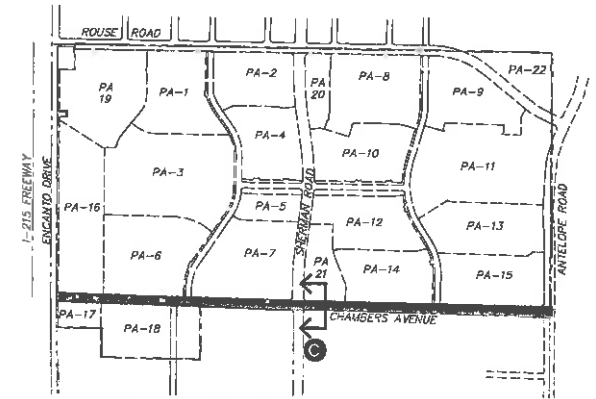


FIGURE 2.11B: CHAMBERS AVENUE

# SECTION 2

## COMMUNITY DEVELOPMENT PLAN

Adjacent to the water quality basin, there will be a three- to four-foot high berm with additional landscaping.



**FIGURE 2.11C: CHAMBERS AVENUE**

# FLEMING RANCH SPECIFIC PLAN

West of Sherman Road the right-of-way widens to 100 feet. Adjacent to the Sports Park, there will be an eight-foot wide curb adjacent sidewalk on the south side.

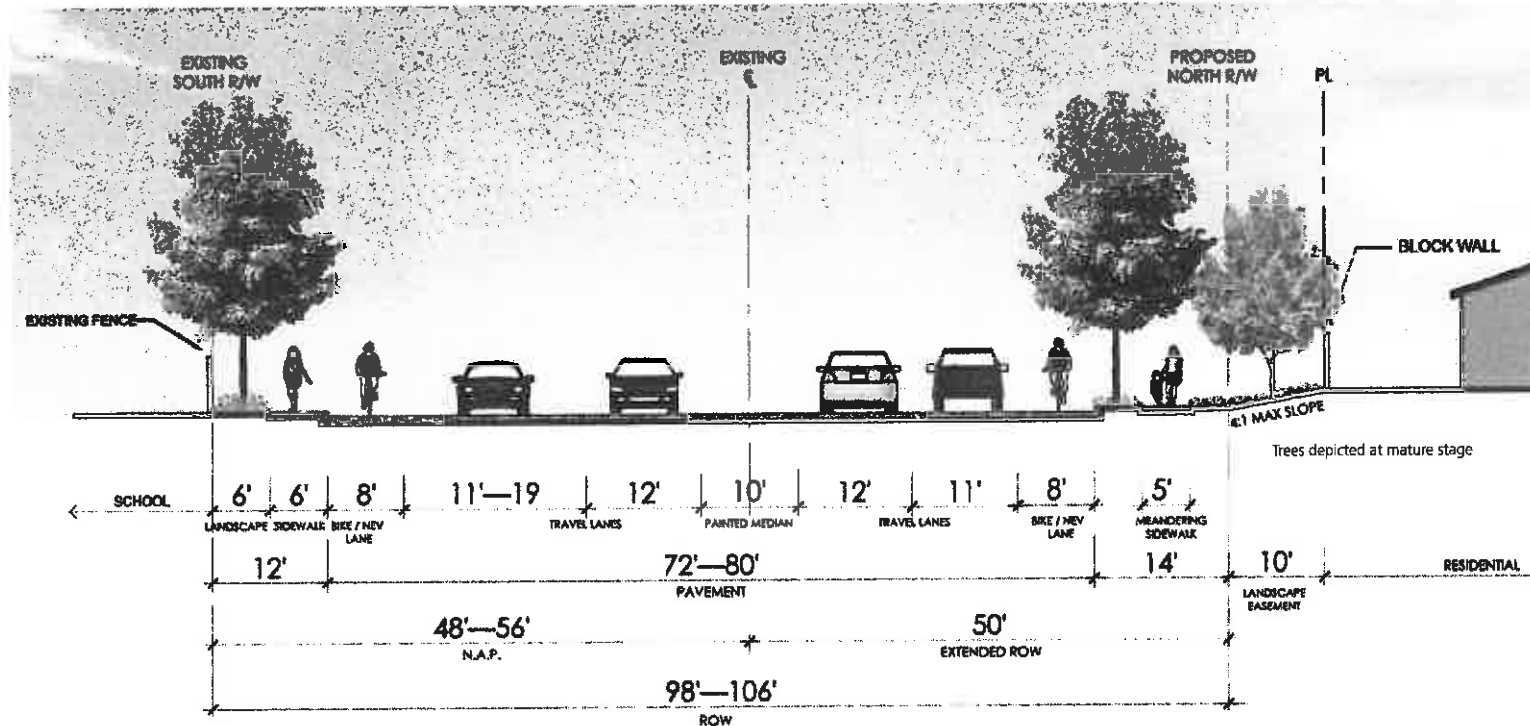
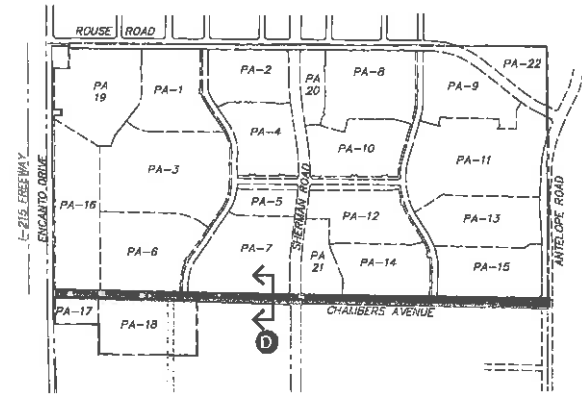
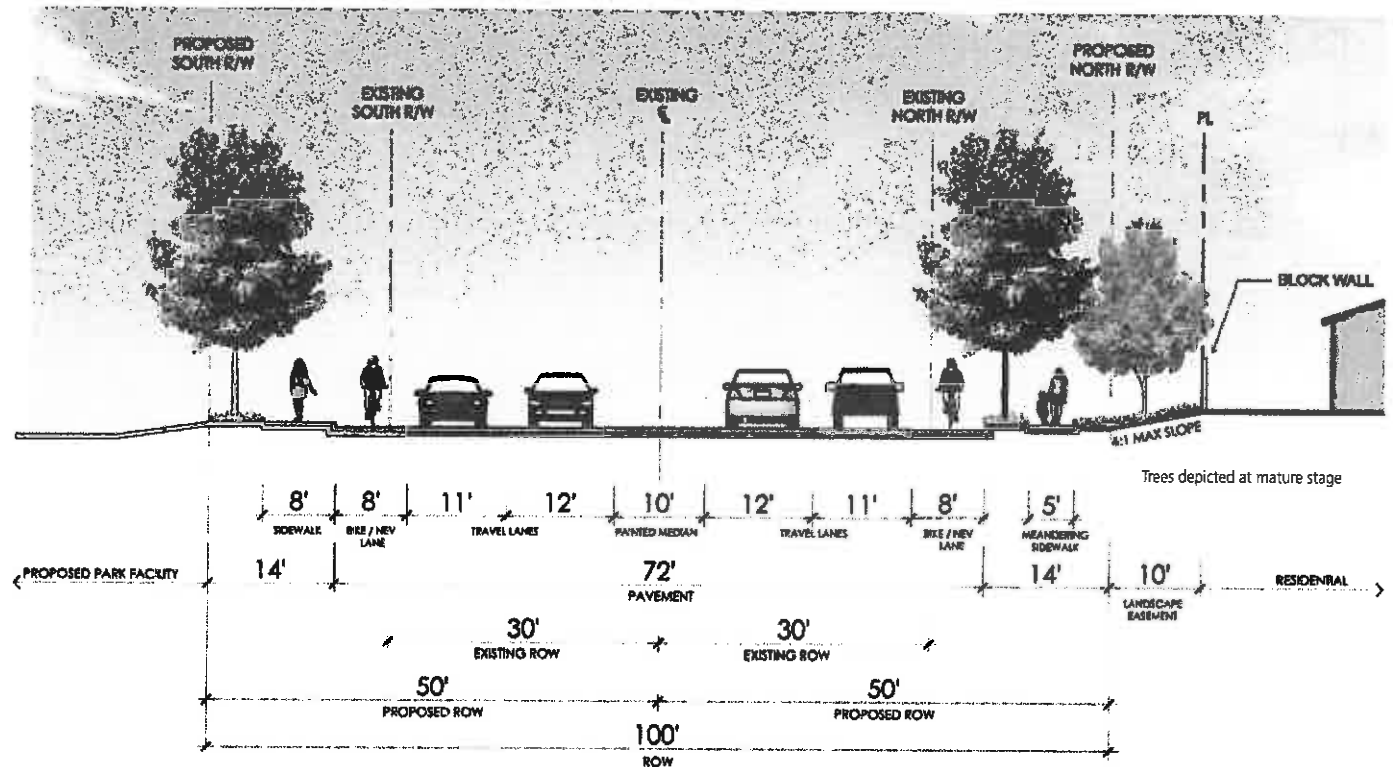
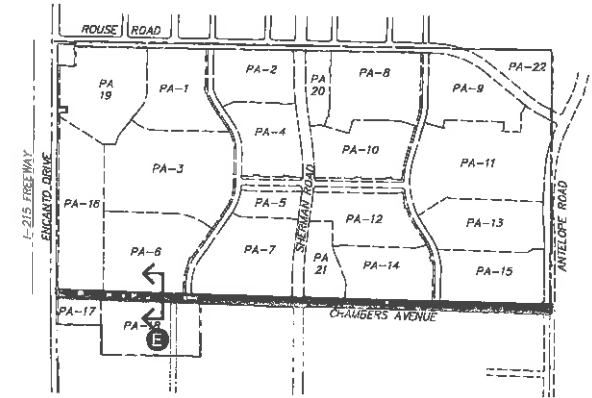


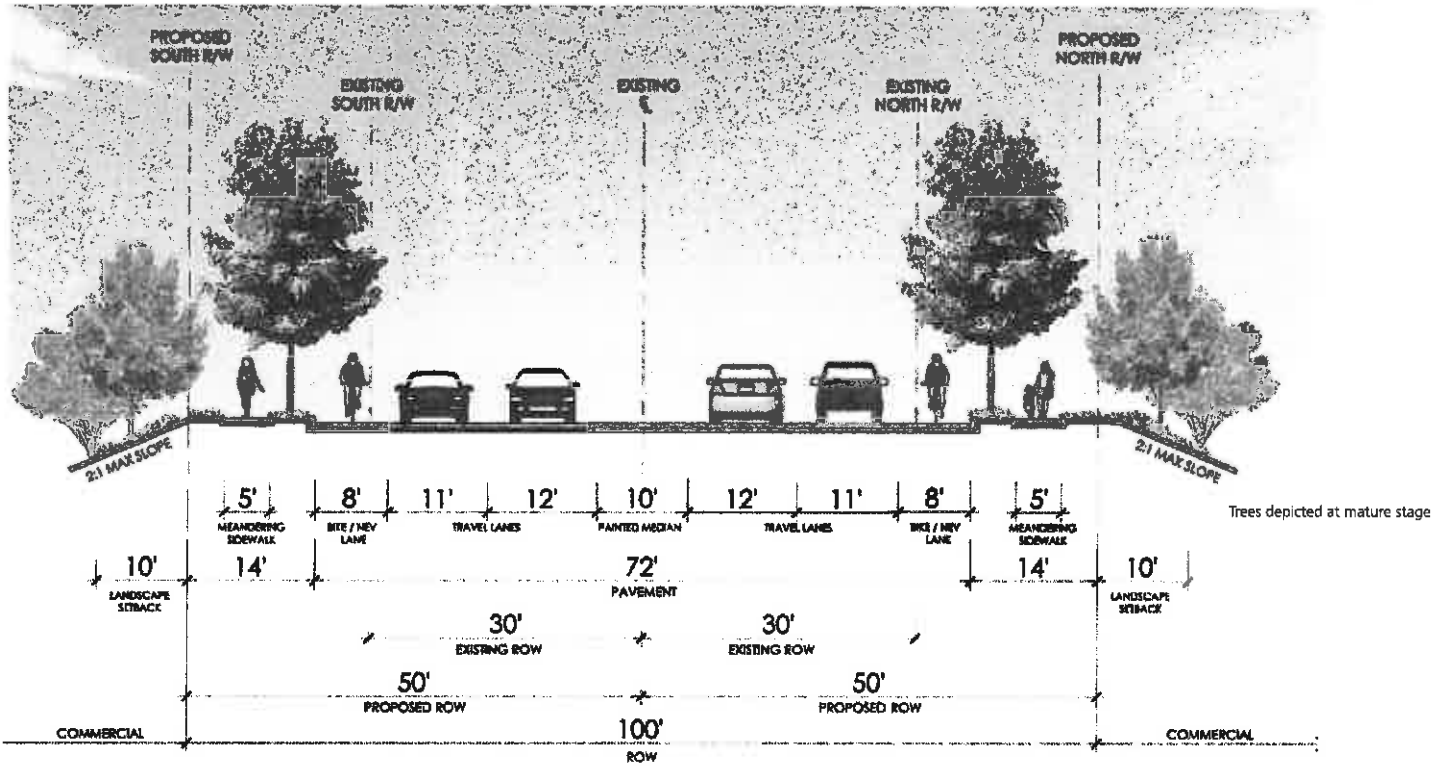
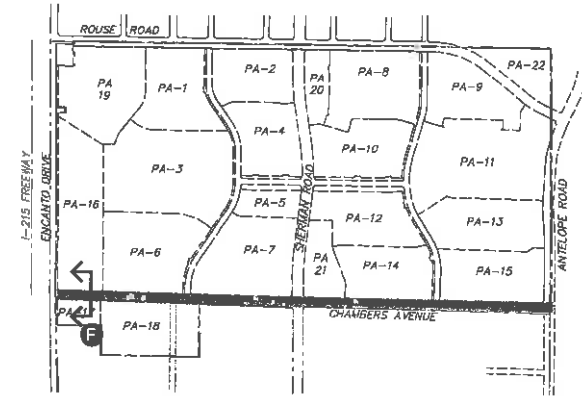
FIGURE 2.11D: CHAMBERS AVENUE

# SECTION 2

## COMMUNITY DEVELOPMENT PLAN



**FIGURE 2.11E: CHAMBERS AVENUE**



**FIGURE 2.11F: CHAMBERS AVENUE**



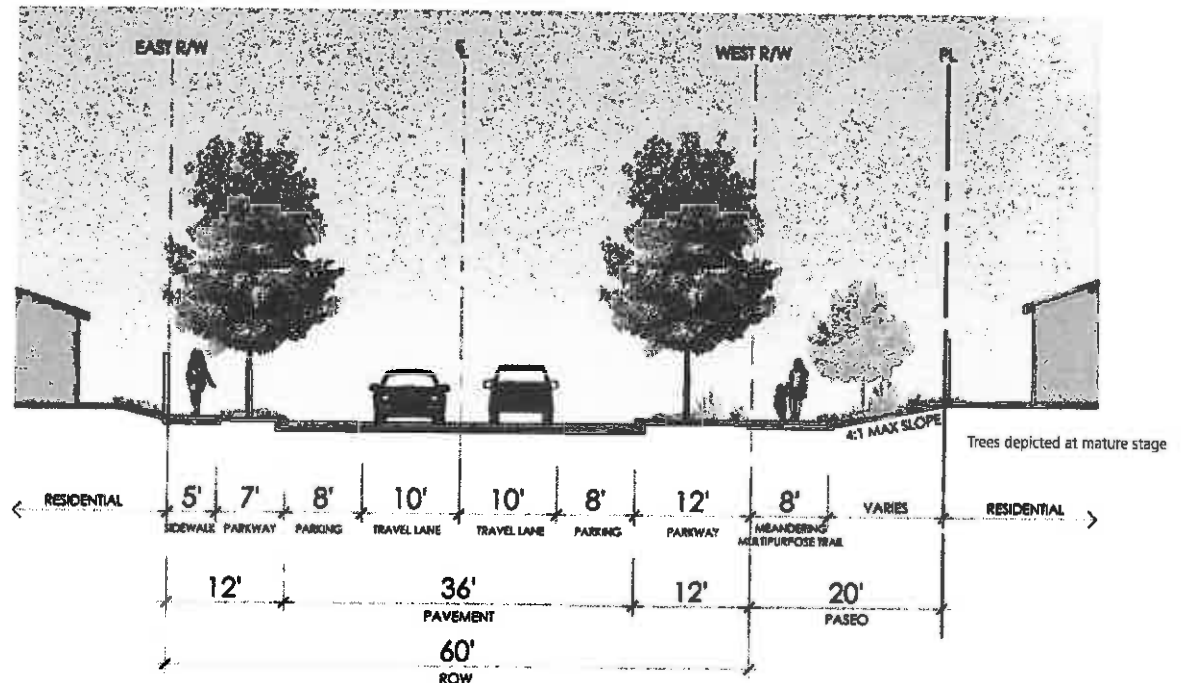
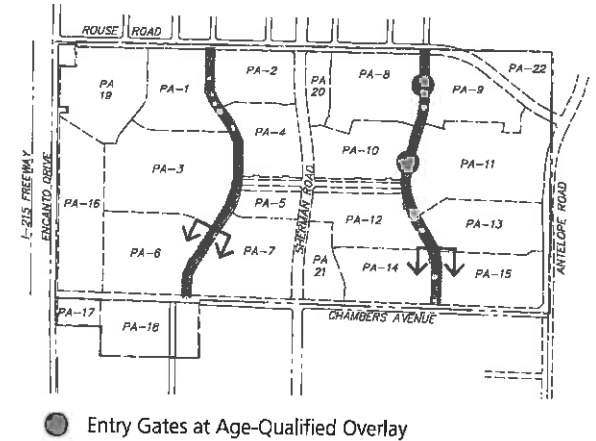
## SECTION 2

### COMMUNITY DEVELOPMENT PLAN

#### F. LOCAL ROAD WITH ENHANCED PASEO

There are two Local Roads with Enhanced Paseos that connect to both Rouse Road and Chambers Avenue. These roads include two travel lanes (one in each direction) and parking on either side. One side of the local road includes a 20-foot wide enhanced paseo with activity nodes with and eight-foot wide meandering trail. The other side of the local road has a 5-wide sidewalk with a 7-foot wide landscape area. Should the East Village be developed under the Age-Qualified Overlay, entry gates shall be constructed just south of Rouse Road and north of the Village Entry (highlighted in blue on the key map). The segment behind the gates would be a privately owned and maintained roadway.

The Local Road with Enhanced Paseo Roadway classification is unique to this SP.

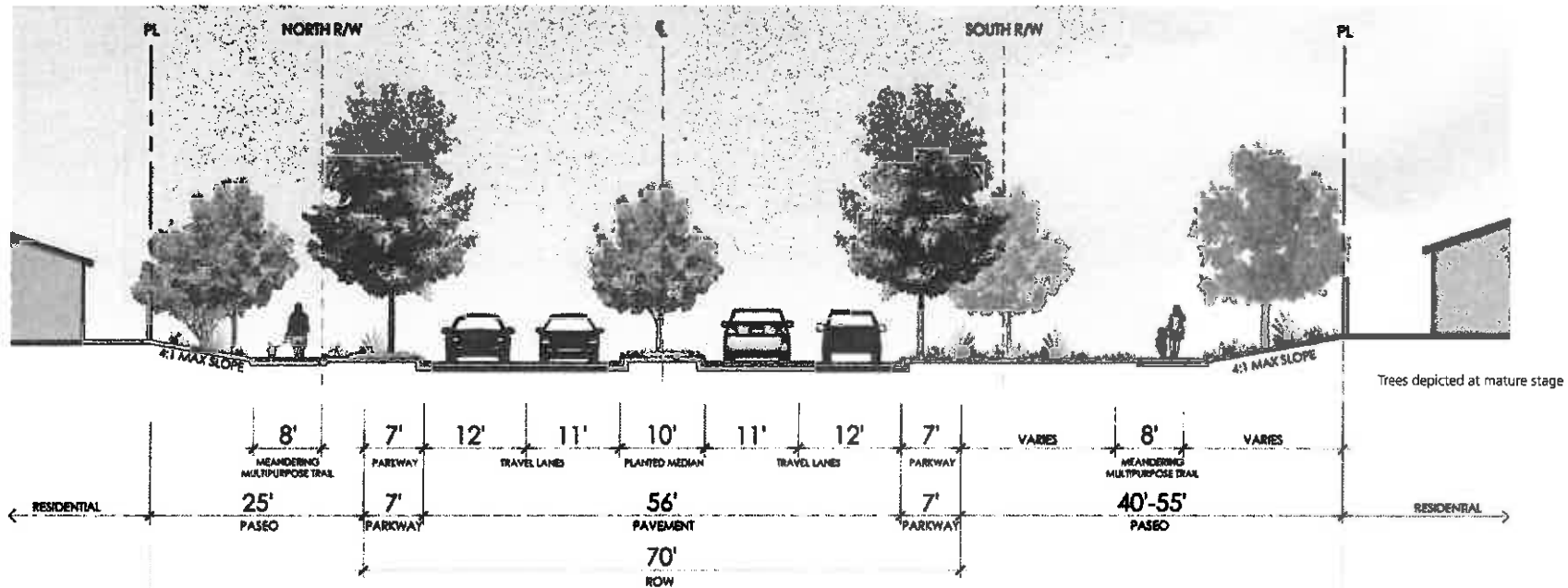
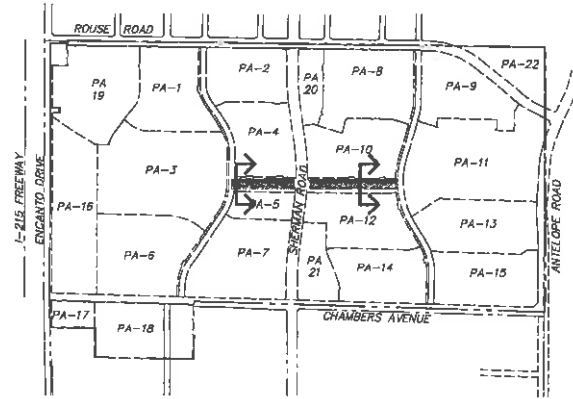


**FIGURE 2.12: LOCAL ROAD WITH ENHANCED PASEO**

**G. VILLAGE ENTRY WITH ENHANCED PASEO**

The Village Entry Roadway is the primary entry road to both the West and East Villages and serves as a major thematic roadway for the community. Designed with four travel lanes (two in each direction) with a ten-foot wide landscaped median, the streetscape and treatment of the Village Entry plays a strong role in defining the overall character for the community. The right-of-way includes a meandering eight-foot wide trail and the 20-foot to 60-foot wide enhanced paseo with activity nodes located on both sides of the roadway providing amenities such as adult exercise stations and tot lots for the residents.

The Village Entry Roadway with Enhanced Paseo classification is unique to this SP.



**FIGURE 2.13: VILLAGE ENTRY**

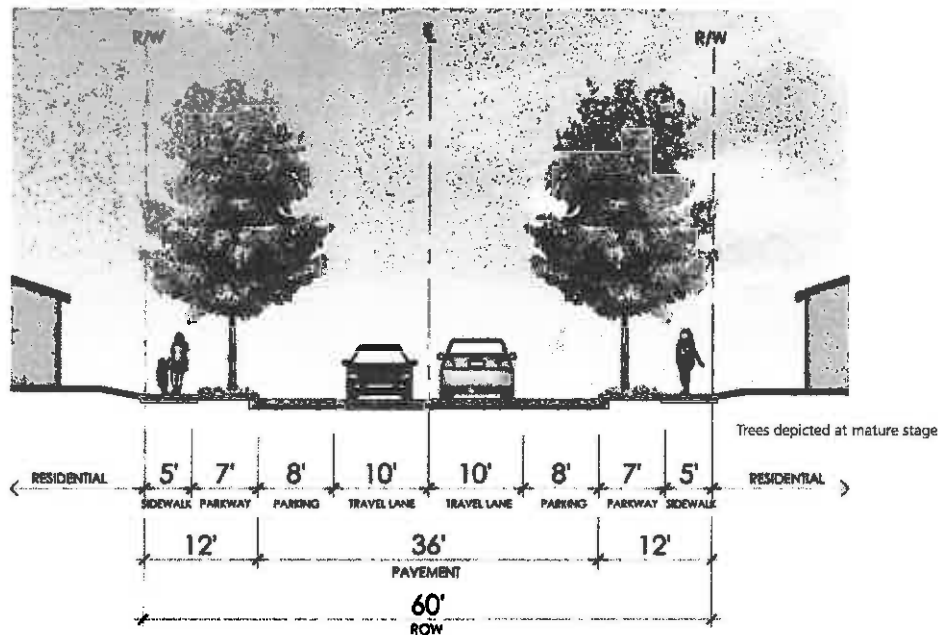
**H. LOCAL ROADS**

The precise location and alignment of Local Roads are not outlined in this SP and shall be subject to approval of subsequent entitlement actions such as Tentative Tract Maps, Parcel Maps, Plot Plans and/or Site Plans.

Local Roads are planned to accommodate two lanes of vehicular travel (one in each direction) and parking on both sides in a 36-foot wide paved area. Additionally, a five-foot wide landscape area adjacent to the curb and a five-foot sidewalk on both sides of the paved roadway are included in the right-of-way. Should the East Village be developed as an age-restricted community, the local roadways behind the gates shall be a privately owned and maintained.

**2.3.2 INTERSECTIONS**

After the Traffic Impact Study has been completed, text will include descriptions of the intersection improvements.



**FIGURE 2.14: TYPICAL LOCAL ROADWAYS**

### **2.3.3 NEV, BIKEWAY AND PEDESTRIAN NETWORK PLAN**

The NEV, Bikeway and Pedestrian Network Plan provides a well-connected network that allows for alternative methods of transportation. As illustrated in Figure 2.15: NEV, Bikeway and Pedestrian Network Diagram, the network provides access to a majority of the residential neighborhoods and community amenities within the SP.

The NEV, Bikeway and Trail network consists of four primary trail types:

- ❖ NEV and Bikeway Lanes
- ❖ Class II Bikeway Lanes
- ❖ Multi-purpose Trails
- ❖ Sidewalks

#### **A. NEV AND BIKEWAY LANES**

Neighborhood Electric Vehicles (NEVs) and golf carts have similar characteristics and can connect into City's network of NEV routes and the Meniffee Bikeway and Community Pedestrian Network to provide a framework for low-speed vehicle usage within the City. Within the SP, the 8-foot wide lanes allow for the Bicycles and NEVs to share the same lane.

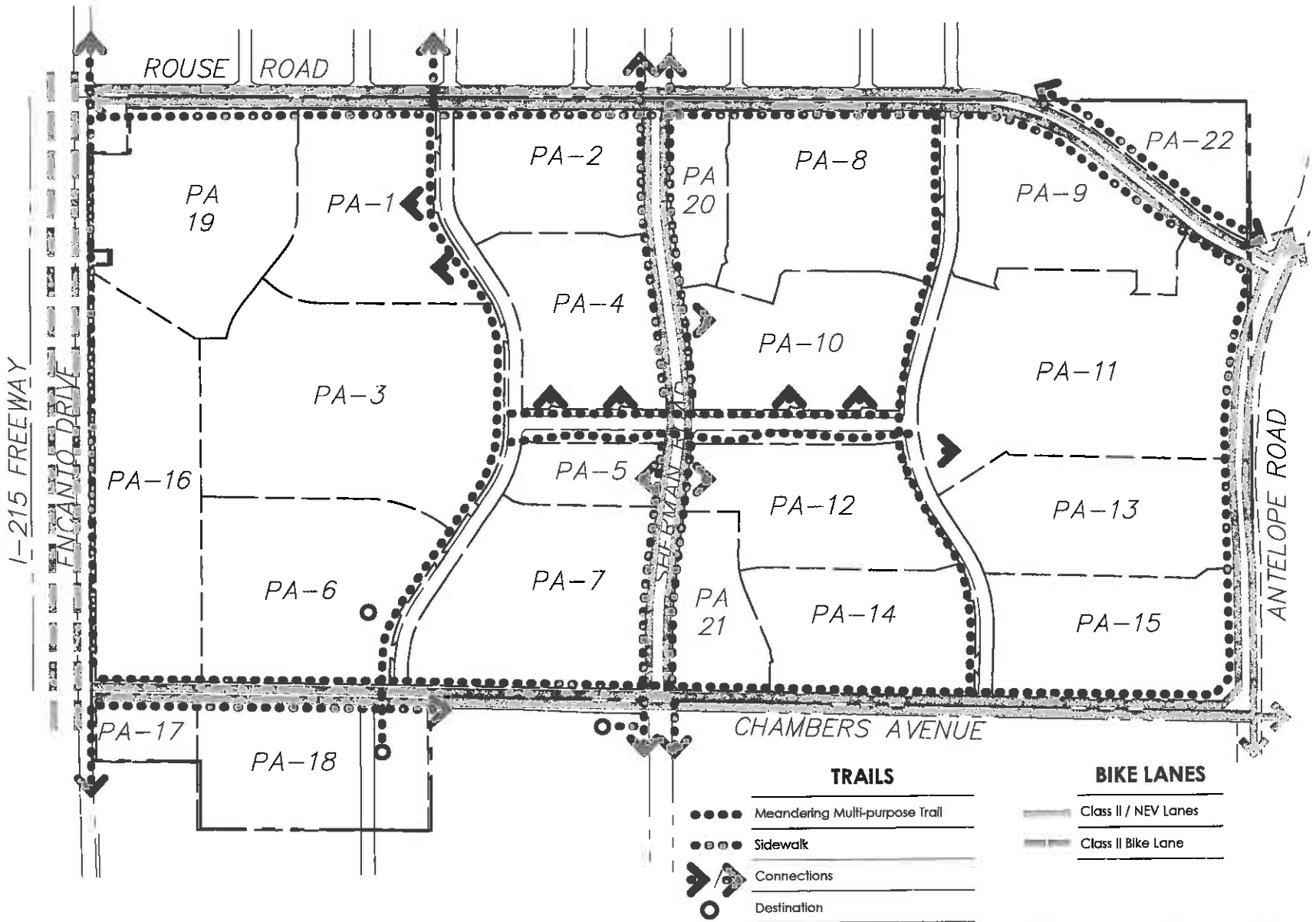
It is important to note that the bike lanes planned within the Fleming Ranch Specific Plan may not immediately connect to bike lanes within existing, adjacent roadways. It is assumed that the master plan bike lanes will be a part of an overall bike lane system once implementation and construction of General Plan roadway system has been completed.

#### **B. PEDESTRIAN CONNECTIONS**

In order to provide maximum connectivity throughout the SP area, trail and sidewalk connections are provided between residential neighborhoods and the enhanced paseos. Crosswalks and enhanced paseos provide meaningful trail connections within the master plan. Meandering eight-foot wide multi-purpose trails within the enhanced paseo consist of either concrete or decomposed granite material and are ADA accessible. All streets include five-foot wide sidewalks where there are no paseos. Refer to Figure 2.15: NEV, Bikeway and Pedestrian Network Diagram for location of the meandering Multi-purpose trails.

A Traffic Impact Analysis (TIA) will determine if a traffic signal is warranted at Chambers Avenue and Sherman Road. If a traffic signal is not warranted, a protected and enhanced crosswalk will be constructed to allow safe crossing between the homes and the school and sport park.

**SECTION 2**  
COMMUNITY DEVELOPMENT PLAN



**FIGURE 2.15: NEV, BIKEWAY AND PEDESTRIAN NETWORK DIAGRAM**

## 2.4 SOILS & TOPOGRAPHY

The majority of the topography of the SP area ranges from 1,425 to 1,511 feet above sea level and consists primarily of flat lands. A knoll exists in the northeastern portion of the SP area which is 1,650 feet above sea level at its highest point and is a constraint to the design of the roadway system in this area.

The property underlying the SP area has been significantly altered over the years by agricultural uses. The SP area consists primarily of grazing land and does not contain prime farmland, unique farmland, or farmland of Statewide significance. Grazing land is not considered to include important soils as classified by the California Environmental Quality Act (CEQA) or the Farmland Protection Policy Act (FPPA).

In addition, the SP area is not located within a 100-year floodplain as identified by the Federal Emergency Management Agency.

As a part of the Specific Plan process, a series of geotechnical evaluations were conducted to determine the suitability of the SP area for development. The following three reports were prepared and are included as appendices in the EIR for this SP.

- ❖ Preliminary Geotechnical Investigation, by Leighton and Associates, Inc., 2005
- ❖ Preliminary Geotechnical Evaluation, by LGC Geotechnical, Inc., 2017
- ❖ Phase 1 Environmental Site Assessment

Based on preliminary geotechnical evaluation, there are no major constraints to development within Fleming Ranch. Evidence of active faulting was not identified and there is a low potential for liquefaction due to the presence of underlying clay and stiff silty layers with dense physical characteristics of granular soil layers. Shallow groundwater is also not expected to be a constraint to site development.

Additionally, a site-specific Phase 1 Environmental Site Assessment was prepared

for this site, and concludes that there are no Recognized Environmental Conditions (RECs), Controlled Recognized Environmental Conditions (CRECs), or Historical Recognized Environmental Conditions (HRECs) that could adversely affect future development of the site with residential, commercial and recreational land uses.

## 2.5 GRADING PLAN

The land development area that makes up the SP falls from east to west in a gentle manner with approximately 255 feet of elevation difference over nearly a mile in length, averaging a 1.6% slope. From north to south approximately 13 feet of elevation difference exists over approximately 3,200 feet resulting in 0.4% slope.

Figure 2.16: Grading Plan follows the form of the existing terrain. Generally, the grading plan proposes a scenario that slopes from east to west and from south to north. As described in the 2.6 Drainage section, the grading plan conforms to a drainage concept that directs storm water runoff to various basins within the SP area which have a dual purpose of water quality treatment and flood water detention. In addition, the basin on the southern boundary of the SP area serves to intercept and detain off-site storm water runoff coming from the southeast while the basin on the west boundary of the project will also intercept additional storm water runoff from the south.

Development of the SP area will generate approximately 1.0 million cubic yards of earthwork volume. Additionally, there will be roughly 1.2 million yards of remedial earthwork volume consisting primarily of over-excavation. In total, development and over-excavation (with adjustment factors such as shrinkage, bulking and subsidence) will generate approximately 2.2 million yards of earthwork volume. The fill earthwork quantities are expected to match the cut earthwork quantities resulting in an overall balanced earthwork operation requiring no importing or exporting of earthwork materials. In general, the grading plan generates mostly cut earthwork operations on the east side of the SP area which provides the needed fill materials on the west side.

# SECTION 2

## COMMUNITY DEVELOPMENT PLAN



FIGURE 2.16: GRADING PLAN

### **2.5.1 GRADING PLAN DEVELOPMENT STANDARDS**

1. All grading activities shall conform to City of Menifee standards, shall be in substantial conformance with the Figure 2.16: Grading Plan, and shall implement any grading-related mitigation measures.
2. Prior to initial grading activities, a detailed soils report and geotechnical study shall be prepared to analyze on-site soil conditions and slope stability and will include appropriate measures to control erosion and dust.
3. All streets shall have a gradient not to exceed 15 percent.
4. Prior to any development within this SP, a detailed rough grading plan for the project shall be submitted to the City of Menifee for approval. The detailed grading plan submitted to the City for approval shall include: techniques employed to prevent erosion and sedimentation as well as eliminate source pollutants during and after the grading process; approximate time frames for grading; and pad elevations and roadway elevations. Grading work shall be balanced on-site whenever possible.
5. All manufactured/graded slopes shall be at a minimum 2:1 gradient and shall follow the recommendation of the geotechnical engineer. A geotechnical report shall be submitted to the City for approval with the rough grading plan. The slope stability report shall also contain recommendations for landscaping and erosion control. City Ordinance No. 457 will be observed regarding setback and landscaping requirements with regard to slopes.
6. Where cut and fill slopes are created higher than three feet, detailed landscaping and irrigation plans shall be submitted to the City for review and approval prior to Grading Plan approval. Plans shall be reviewed for type and density of ground cover, shrubs, and trees.
7. The applicant shall be responsible for maintenance and upkeep of all planting and irrigation systems until those operations are the responsibilities of other parties.
8. Potential brow ditches, terrace drains, or other minor swales, determined necessary by the City of Menifee shall be lined with natural erosion control materials or concrete.
9. A grading permit shall be obtained from the City of Menifee, as required by the City Grading Ordinance, prior to any grading operations.
10. If any historic or prehistoric remains are discovered during grading, a qualified archaeologist should be consulted to ascertain their significance, as specified in the project EIR. If human remains are discovered, work shall halt in that area and procedures set forth in the California Public Resources Code (Sec. 5097.98) and State Health and Safety Code (Sec. 7050.5) shall be followed, including notification to the County Coroner. If Native American remains are present, the County Coroner shall contact the Native American Heritage Commission to designate a Most Likely Descendant.



11. Pursuant to requirements of the State Water Resources Control Board, a statewide general NPDES construction permit will apply to construction activities associated with the proposed project to prevent erosion and sediment/pollutant transport off-site during rain events. Construction activities covered under the permit include cleaning, grading, or excavation that results in the disturbance of at least five acres of total land area or activity which is part of a larger common plan of development of five acres or greater. Therefore, the developer or builder of the project shall be required to obtain coverage under the appropriate state NPDES permits prior to commencing grading activities.
12. Anything to the contrary, proposed by this Specific Plan, shall not supersede the following: All grading shall conform to the California Building Code, City of Menifee General Plan, Ordinance 457 and all other relevant laws, rules and regulations governing grading in the City of Menifee.

## 2.6 DRAINAGE

### 2.6.1 EXISTING CONDITIONS

The SP area lies within a watershed that is part of Riverside County's Santa Ana Region. The SP area is a seasonally disked grassland that generally drains from east to west in a sheet-flow manner averaging a 1.6% grade over one mile in length. From north to south approximately 13 feet of elevation difference exists over approximately 3,200 feet resulting in 0.4% slope. As highlighted on Figure 2.17: Drainage Plan, flows originating from within the SP area (and adjacent tributary area) ultimately drain to the existing Caltrans 10-foot x 5-foot reinforced concrete box (RCB) culvert at the western edge of the SP boundary which conveys the existing flows under Encanto Road and the I-215 Freeway to an existing Riverside County Flood Control and Water Conservation District maintained concrete trapezoidal channel (Channel G-G) on the opposite side. This trapezoidal channel continues in a southwestern direction, while also accepting additional flows from other drainage areas and ultimately outlets in the regional Salt Creek Channel which then flows to Canyon Lake.

The total on-site and off-site tributary area is 818 acres in the existing condition. This includes the on-site area and off-site areas to the north, east, southeast and south as follows:

- ❖ 379.2 acres on-site and east of the SP area (draining directly to the existing Caltrans RCB culvert on the westerly side of the SP Area);
- ❖ 106.5 acres off-site to the north (currently draining to existing Encanto Road via dual 51-inch storm drains and rectangular channel on west side of Encanto);
- ❖ 214.2 acres off-site to the southeast (Menifee Valley Medical Center and adjacent tracts currently draining overland through the SP area and being discharged via box culvert into the SP Area); and
- ❖ 119 acres off-site to the south (currently draining to Encanto Road directly to the existing Caltrans RCB culvert).

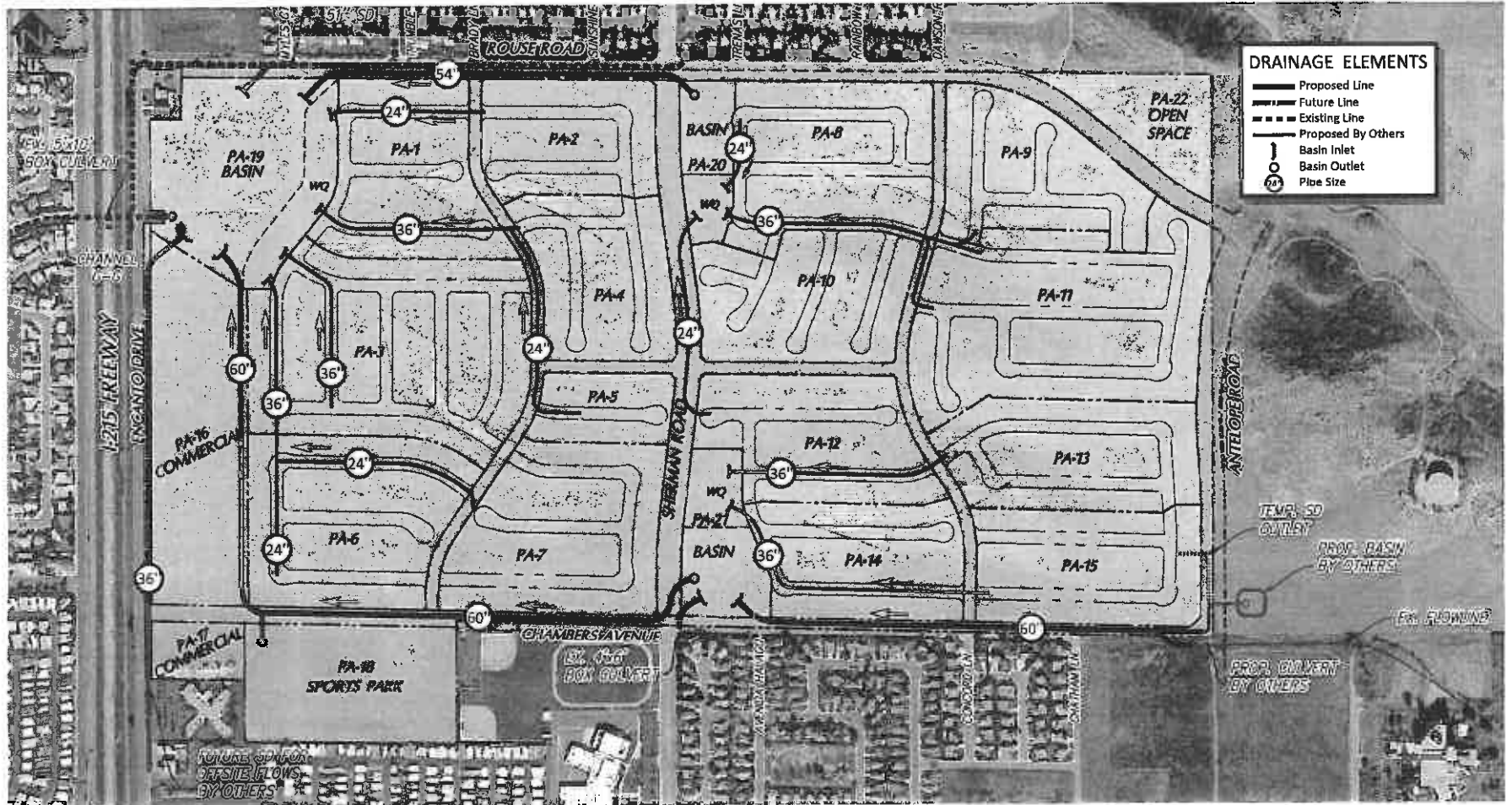
The total flow rate of the existing undeveloped 100-year, 3-hour storm flow is 1,174 cubic feet per second (cfs) at the most downstream outlet of the site at the Caltrans RCB crossing Encanto Road. The capacity of this culvert using existing Encanto Road is approximately 450 cfs, which limits collection of upstream surface water. As such, the developed condition of the SP provides mitigation measures to decrease flows to the acceptable capacity of the existing downstream facility.

Additionally, Encanto Road has an extremely flat grade, with less than 0.2% grade in some sections from south to north, which conveys off-site storm flows along Encanto Road from parcels (both developed and vacant) located south of the SP Area (between the southern boundary of the SP Area and McCall Boulevard). A majority of Encanto Road is not constructed with curbs and therefore utilizes graded swales west of the road to convey storm water drainage. This condition is highly inadequate and creates seasonal flooding during large storm events along the segment of Encanto Road that forms the western SP Area boundary.

Currently, storm water flows for the entire southerly tributary area (119 acres) are routed in a northerly direction along Encanto Drive, are then conveyed under the freeway at the existing Caltrans RCB and then drain southward toward the Salt Creek channel via the Sun City Golf Course. Storm water flows from the north are captured in double 51-inch Reinforced Concrete Pipes within the Rouse Road right-of-way and then conveyed via an open rectangular concrete channel along the western edge of Encanto Drive to the existing Caltrans RCB culvert. Furthermore, storm water flows from the southeast travel across McCall Boulevard and through the Menifee Valley Medical Center property to an earthen channel which outlets into the adjacent tract developments to the south of the SP area. These storm water flows proceed to travel overland through the local streets and eventually are captured in a storm drain system and which outlets via a 6-foot x 4-foot RCB culvert at the intersection of Chambers Avenue and Sherman Road into the SP Area.

# SECTION 2

## COMMUNITY DEVELOPMENT PLAN



**FIGURE 2.17: DRAINAGE PLAN**

These storm water flows along with the existing RCB culvert and the very flat grades create a storm flow capacity challenge for development within the SP. To address this, there are two primary drainage goals: 1) Safely convey on-site and off-site flows through the SP area to the existing Caltrans RCB culvert under I-215 to the existing RCFCWCD Channel G-G; 2) Mitigate the SP outlet flow rates to the acceptable capacity of the existing downstream system.

The following sections describe the primary elements that make up the Drainage Plan and how the overall system will safely convey the flows through the SP area. In addition, details for the detention basins are provided and describe how they will decrease storm water flows, while also providing water quality treatment.

### **2.6.2 OVERALL DRAINAGE AREA CONVEYANCE SYSTEM (DEVELOPED CONDITION)**

As previously mentioned, the SP Area has off-site drainage areas that are tributary to the site. For the 106.5-acre off-site tributary area north of the SP Area, the existing 51-inch dual storm drain conveyance system in Rouse Road will be rerouted to drain into the westerly detention basin instead of inletting directly to the existing Caltrans RCB culvert via a rectangular channel to the west of Encanto Drive. Due to the widening of Encanto Drive, the existing rectangular channel to the west of the road will remain in place.

For the 214.2-acre off-site tributary area southeast of the SP area, the storm water runoff will be intercepted at the perimeter of the SP area at two locations (near the intersections of Chambers Avenue/Antelope Road and Chambers Avenue/Sherman Road) and conveyed via underground storm drain pipe to the southerly detention basin. In addition, the outlet pipe from the detention basin for the 43.4-acre proposed development area east of the SP area will also be conveyed to the southerly basin.

The remaining 119 acres of off-site area to the south and along Encanto Road will be intercepted from draining directly into the Caltrans RCB culvert and be conveyed to the SP area via a new storm drain system within or adjacent to Encanto Drive. This underground piped system collects flows at the low points and conveys them northerly to the proposed westerly detention basin.

### **2.6.3 SPECIFIC PLAN AREA CONVEYANCE SYSTEM (DEVELOPED CONDITION)**

In general, Figure 2.17: Drainage Plan, shows the storm drain conveyance to multiple basins that serve as both a water quality and flood detention facilities. The northeast quarter of the SP area uses an underground storm drain system to collect storm water flows and convey them to the on-site northerly detention basin. The southeast quarter of the SP area uses an underground storm drain system to collect storm water flows and convey them to the on-site southerly detention basin. The westerly half of the SP area uses an underground storm drain system to collect storm water flows and convey them to the on-site westerly detention basin. These multiple systems are designed to safely convey and contain 100% of the 100-year storm event flows within the storm drain pipe once collected from the street via curb opening catch basins.

The storm drain network also includes mainline pipes that intercept and convey off-site flows. All of the pipes intercept water at the perimeter of the SP Area. Off-site areas that are intercepted include:

- ❖ The Encanto Drive storm drain which intercepts off-site flows at the southwest SP area boundary corner and conveys them to the westerly detention basin;
- ❖ The Chambers Avenue storm drain which intercepts flows from the proposed adjacent development to the east of the SP area and the existing flows from the southeast and the developments to the south of Chambers Avenue and east of Sherman Road and conveys them to the southerly detention basin; and

- ❖ The Rouse Road storm drain which intercepts flows from the north and will be reconfigured to discharge into the westerly detention basin within the SP area.

#### **2.6.4 DETENTION BASINS & HYDROMODIFICATION (DEVELOPED CONDITION)**

The multiple detention basins within the SP area serve many purposes including: water quality treatment, detaining increased runoff due to development (Hydrological Conditions of Concern (HCOC) mitigation), detention of off-site runoff coming to the SP area, mitigating flooding in Encanto Drive, and reducing the peak runoff flow rate to a level that can be conveyed by the existing Caltrans RCB culvert that runs under I-215.

The on-site detention basins are in series with the northerly and southerly detention basins upstream of the westerly detention basin. Both the northerly and southerly basins serve to treat the water the easterly half of the SP area and contain both the on-site and off-site 100-year flood volume while reducing the peak flow rates to more manageable levels. The northerly detention basin is designed to reduce the peak flow by approximately 155 cfs and detain approximately 10.8-acre-feet of flood volume. The southerly detention basin is designed to reduce the peak flow by approximately 350 cfs and detain approximately 27.9-acre-feet of flood volume. The outlets for the northerly and southerly basins discharge directly into the westerly basin detention area.

The westerly detention basin site is located in the lowest elevations of the SP area in the northwest corner near the intersection of Encanto Road and Rouse Road. The primary purpose of this basin is to mitigate the increased runoff from the development, mitigate the off-site runoff that comes into the SP area, detain the peak storm water flows so they do not exceed the capacity of the existing

RCB culvert under I-215 and treat the water from the westerly half of the SP area. This detention basin is designed to reduce the peak flow by approximately 455 cfs and detain approximately 51.6-acre-feet of flood volume. As a result, the total peak flows to the RCB is approximately 422 cfs.

The westerly detention basin area consists of a sloping bottom and 4:1 side slopes and is designed to temporarily detain or mitigate the increased runoff from the SP area as well as unacceptable capacity volumes to the downstream system. Only during larger storm events will the water quality basin spill over the berm and utilize the capacity of the detention basin. Maximum basin depths are 7 feet to 8 feet furthest downstream and 3 feet to 4 feet upstream. It is expected that the detention basin will be fully drained and dry within a 24 to 72 hour period.

#### **2.6.5 WATER QUALITY**

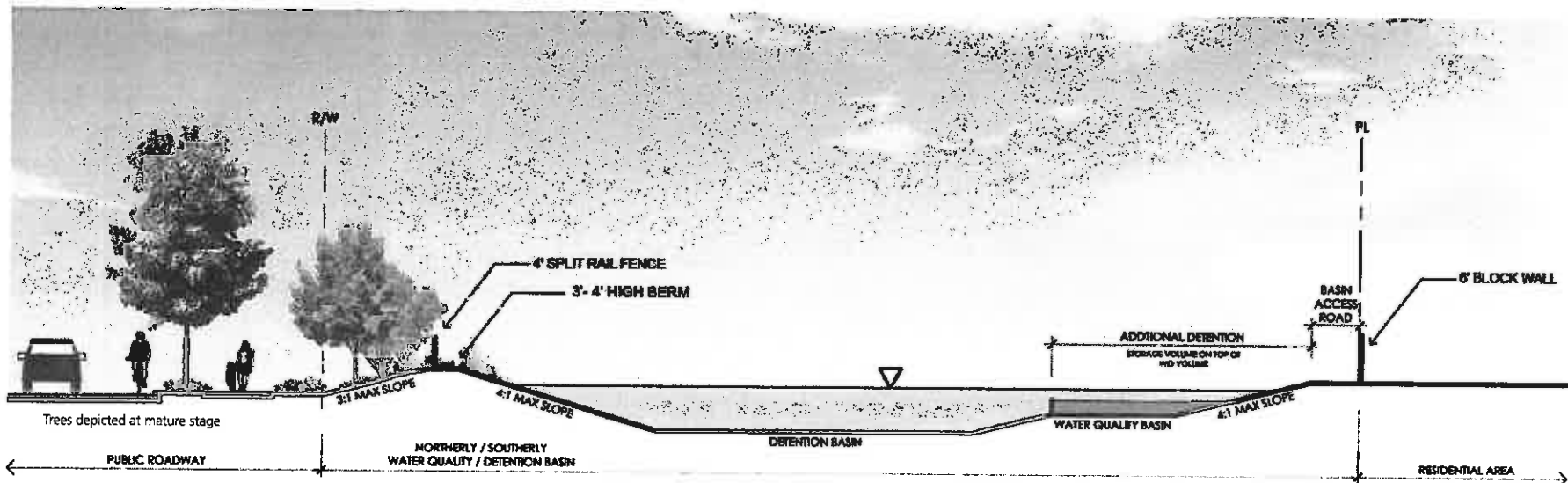
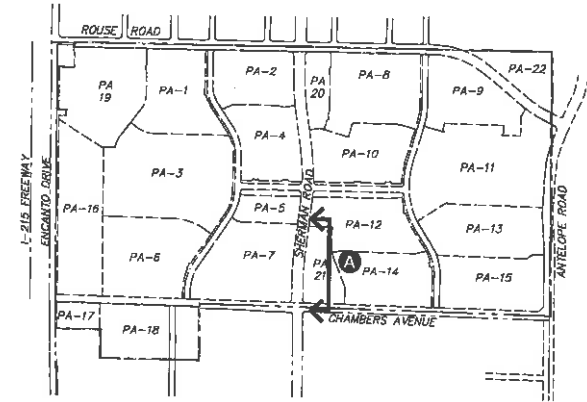
A Water Quality Management Plan (WQMP) shall be prepared for the SP area to identify pollutants and best management practices (BMPs) to treat those pollutants. The WQMP shall conform to the Santa Ana Region WQMP template and be approved by the City of Menifee. The preparation and approval of the WQMP will make the SP area in compliance with Santa Ana Regional Water Quality Control Board (Santa Ana Regional Board) requirements for Priority Development Projects. These requirements are specified in the National Pollutant Discharge Elimination System (NPDES) Municipal Separate Storm Sewer System (MS4) permit issued to the Riverside County Flood Control and Water Conservation District, County of Riverside, and other Cities within the Santa Ana River watershed in the 2010 MS4 Permit. The area covered by this MS4 Permit is referred to as the Santa Ana Region (SAR).

# FLEMING RANCH

## SPECIFIC PLAN

The three basins within the SP area contain both detention and water quality features. The detention features are described in Section 2.6.4. Each basin has a distinctive area for water quality treatment and for detention. However, during large storm events, both areas can be used for detention purposes.

Both the northerly and southerly basin have approximately a 1-acre area reserved for water quality treatment. This area is sized to retain and treat 100% of the easterly half of the SP area's tributary water quality volume need. The westerly basin water quality area consists of an approximately 2.5-acre water quality treatment zone located in the eastern portion of the basin. This area is sized to retain and treat 100% of the westerly half of the SP area's tributary water quality volume need.

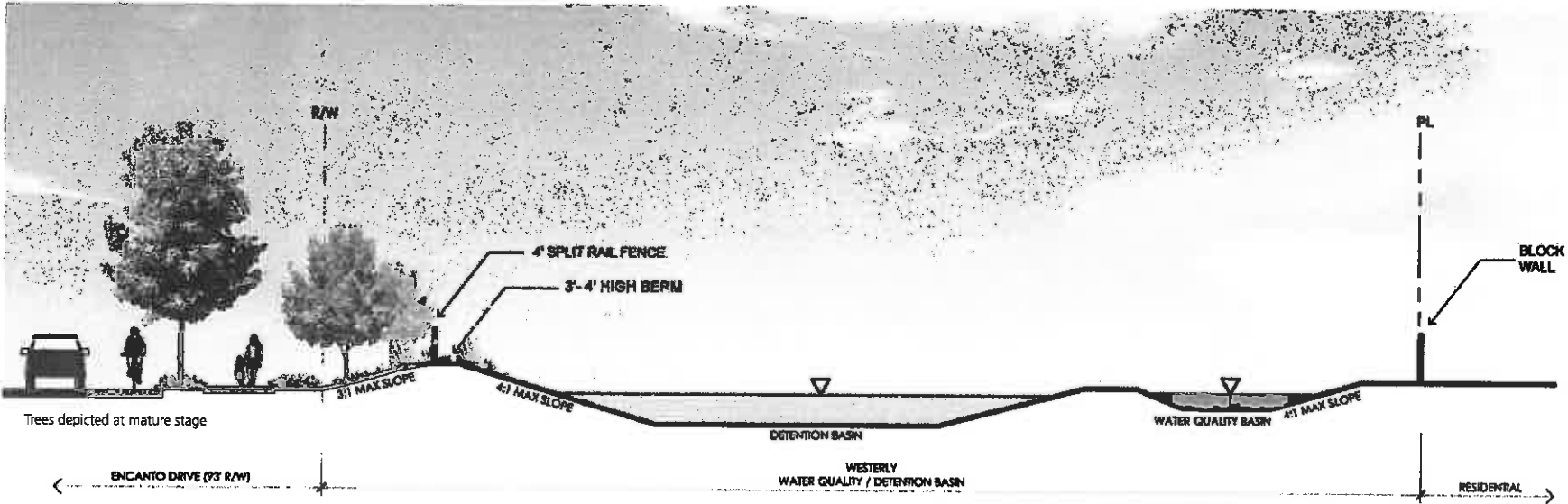
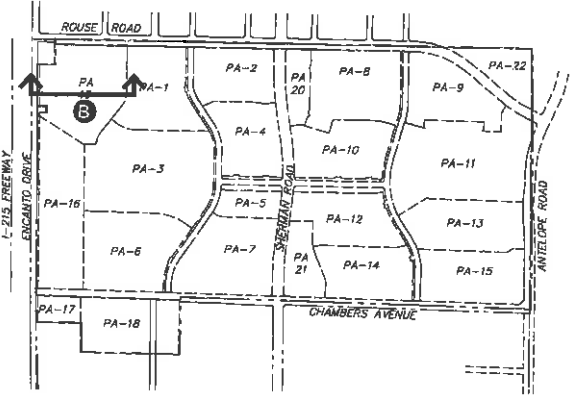


**FIGURE 2.18A: NORTHERLY AND SOUTHERLY CONCEPTUAL BASIN SECTION FOR PA 20 & 21**

# SECTION 2

## COMMUNITY DEVELOPMENT PLAN

The water quality areas are separated from the detention areas by either a berm or a small retaining wall. They are designed to temporarily retain water and slowly release it over a 48- to 72-hour period while allowing time for particles and associated pollutants to settle out. The floor of the water quality areas will be level and will consist of an engineered soil media which will serve as a pollutant filter. Due to the low infiltration rates, subdrains will be placed beneath the engineered soil media to convey the treated water to the detention basin area. Refer to Figure 2.18A: Northerly and Southerly Conceptual Basin Section for PA 20 & 21 and Figure 2.18B: Westerly Conceptual Basin Section for PA 19.



**FIGURE 2.18B: WESTERLY CONCEPTUAL BASIN SECTION FOR PA 19**

## 2.7 POTABLE WATER PLAN

The Fleming Ranch Specific Plan area lies within the Eastern Municipal Water District (EMWD). The project is located within EMWD's 1627 Pressure Zone (PZ). The Menifee Village 1627 PZ storage tank is located south of the SP area, with a high water elevation of 1627 feet. Existing pipelines surround the SP area on the west, south, and north. An 18-inch pipeline runs within the right-of-way of Encanto Road and connects to two 12-inch pipelines located within the rights-of-way of Chambers Avenue and Rouse Road.

Figure 2.19: Potable Water Plan shows the water system consisting of a network of 8-, 12- and 18-inch diameter pipelines. Pipelines located within the right-of-way for Encanto Drive are sized at 18 inches. Pipelines located within the rights-of-way of Antelope Road, Rouse Road, Sherman Road, and internal collector roads are sized at 12 inches. Pipelines located within the rights-of-way for internal local roads are sized at 8 inches.

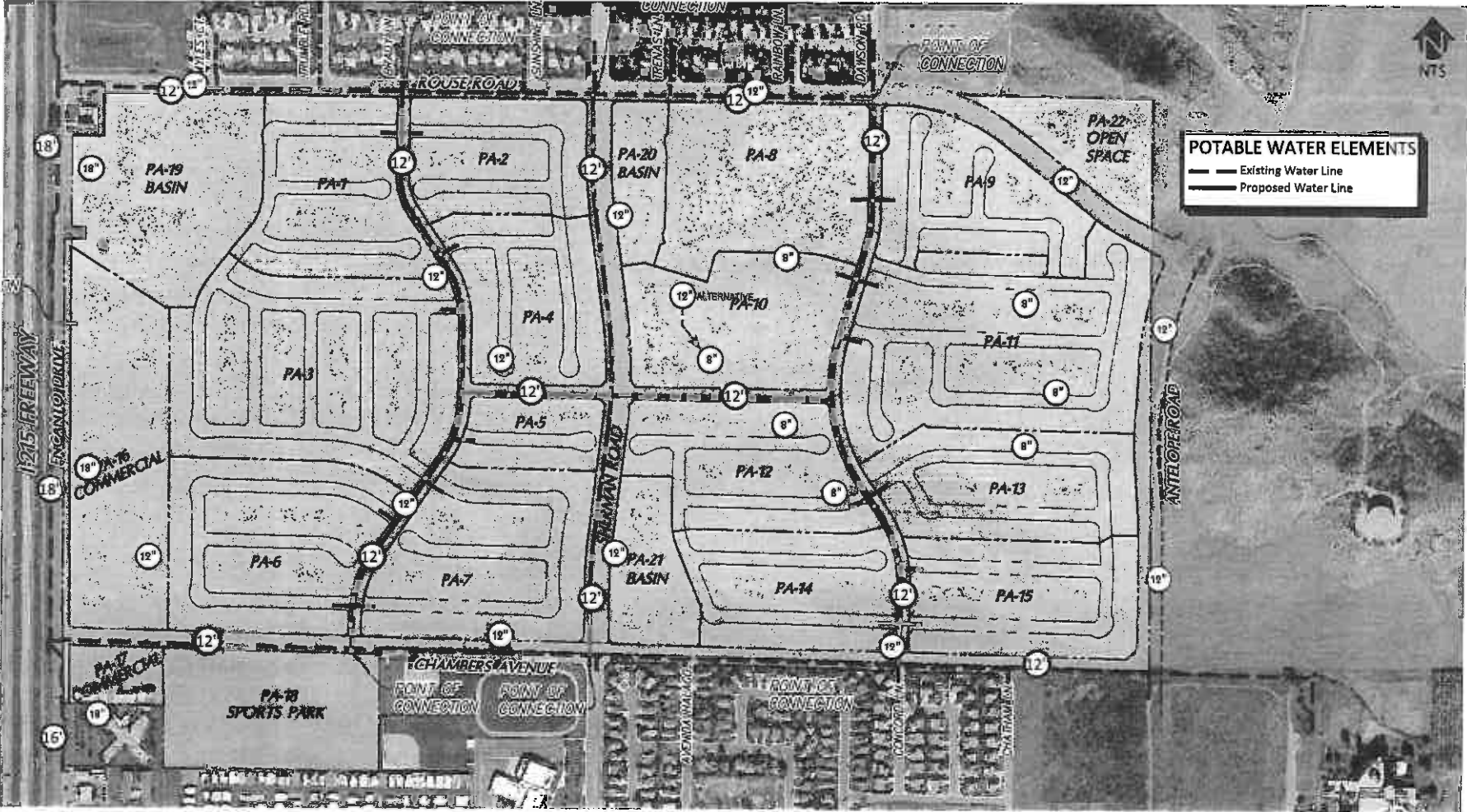
Three connection points are planned to the existing 12-inch pipeline in the Chambers Avenue right-of-way and three connection points are planned to the existing 12-inch pipeline in the Rouse Road right-of-way. Lateral connections to the existing 18-inch pipeline in Encanto Drive are planned to serve the proposed commercial areas.

Demands were calculated for the project based on the SP's proposed land uses and EMWD demand factor criteria. In summary, the site as a whole has a proposed average daily demand of 711,550 gallons per day with peak hour demands at 1,730 gallons per minute.

The fire flow requirement for the residential areas are assumed to be 1,500 gpm, while requirements for the potential recreation center area in the Age-Qualified Overlay is assumed to be 3,000 gpm (further hydraulic analysis must be performed by the developer and approved by EMWD to confirm the water pipeline sizes indicated in this Overlay area on Figure 2.19: Potable Water Plan). A Conditional Plan of Service with EMWD is being processed concurrently with the SP; however, approval of a Final Plan of Service is required prior to final design.



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**FIGURE 2.19: POTABLE WATER PLAN**

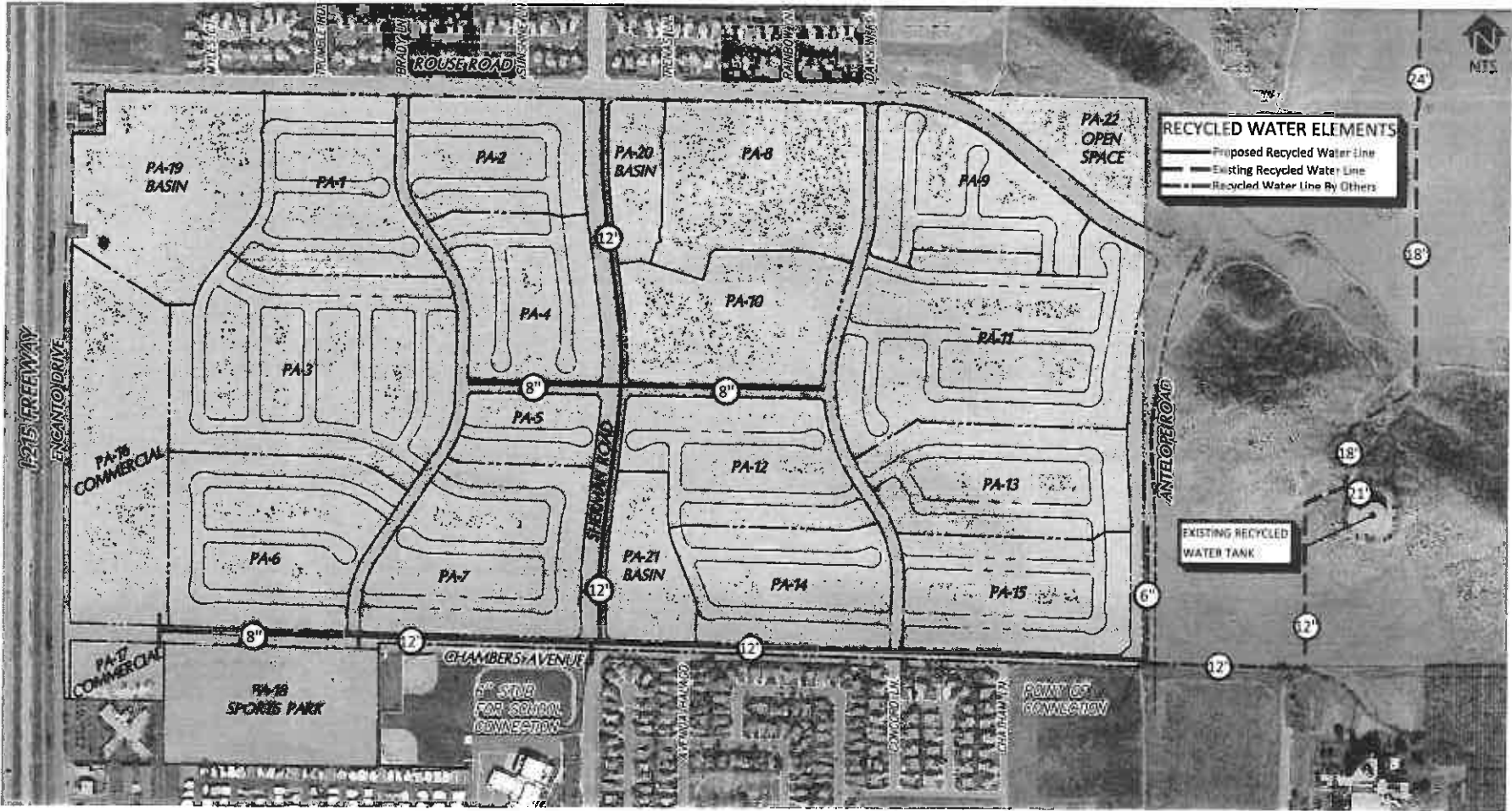
## 2.8 RECYCLED WATER PLAN

Fleming Ranch lies within the EMWD service area for recycled water use. As previously outlined, a number of public landscaped areas and parks are proposed. At major intersections, landscaped development monument areas are planned, as well as landscaped paseos along a majority of the primary roadways. An active park and landscaped community enhanced paseo system are planned throughout the SP area. A private recreation center may be located in the Age-Qualified Overlay. All of these landscaped and recreational amenities are expected to be irrigated with recycled water.

The closest existing recycled water supply source pipeline (and recycled water storage tank) is located east of the SP area, running north-south approximately 700 feet off-site. This system is part of the 1627-zone recycled water system. An extension of the 8-inch water pipeline system, commencing from an existing 12-inch pipeline approximately 700 feet east of the intersection of Chambers Avenue and Antelope Road is proposed to be constructed. Portions of this off-site pipeline are designated to be constructed by the adjacent development per their approved tentative map to provide a connection point at the intersection of Chambers Avenue and Antelope Road.

Figure 2.20: Recycled Water Plan shows the recycled water system proceeding south within the Chambers Avenue right-of-way towards Sherman Avenue. At Sherman Avenue, the pipeline splits and heads west along the right-of-way of Sherman Avenue and continues south within Chambers Avenue. The pipeline within Chambers Avenue will serve the existing school which is currently operating on only potable water but is dual plumbed. The pipeline will also serve the proposed public park west of the school and the commercial areas. The pipeline within the Sherman Road right-of-way will split at the village entry streets and head east and west to serve the proposed landscaped areas, passive parks and potential recreation center.

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**COMMUNITY DEVELOPMENT PLAN**



**FIGURE 2.20: RECYCLED WATER PLAN**

## 2.9 SEWER PLAN

The Fleming Ranch Specific Plan lies within the EMWD. The SP area consists primarily of residential planning areas, a commercial area on the western boundary and a sports park to the south. The project consists of approximately 1,100 proposed DUs creating approximately 415,000 gallons per day with peaked gallons per day at approximately 845,000 gallons.

The general downstream connection point is located within the Encanto Road right-of-way approximately 1,250 feet north of Rouse Road. At this location, the existing sewer system consists of a 15-inch mainline stub-out from an existing manhole. Since there are no other mainline intersections with this 15-inch system for some distance downstream, it appears that this 15-inch stub-out was constructed to provide sewer service for the Fleming Ranch development.

The existing topography drains from southeast to northwest. Consequentially, Figure 2.21: Sewer Plan shows the sewer system following a similar flow pattern. The infrastructure sewer system conveys flows from east to west through a 12-inch to 15-inch system that moves through the middle of the SP area. The north and south halves of the easterly SP area, flow to the center and then flow west and north. The westerly SP area flows primarily west, ultimately connecting to the 15-inch line located within the Rouse Road right-of-way. At this point the 15-inch sewer line heads west on Rouse Road and parallels an existing system for a short length before it heads north again within the Encanto Road right-of-way. This new line connects to the existing 15-inch mainline stub-out located within the Encanto Road right-of-way.

As previously mentioned, the natural grade slopes from east to west. From south to north there is minimal, if any, slope gradation. As such, the south to north sewer system located within the west portion of the SP area runs at the EMWD specified minimum grade of  $S=0.0024$  for 12-inch pipes. The 15-inch sewer pipe within the Encanto Drive right-of-way runs at the minimum 0.0016 all the way to the connection point. Although this grade is minimal, it is required due to the extreme lack of grade from south to north.

In all cases, the depth of flow does not exceed the maximum capacity standards of half full for pipes 12-inch and smaller as well as three-quarters full for pipes 15-inch or larger. As shown on Figure 2.21: Sewer Plan, the northern most part of the system begins with an 8-inch sewer main, then increases in size to 12 inches and 15 inches as needed when capacities hit their specified limits. The northern most part of the system begins with an 8-inch sewer main, then increases in size to 12 inches and 15 inches as needed when capacities hit their specified limits.

**SECTION 2**  
**COMMUNITY DEVELOPMENT PLAN**



**FIGURE 2.21: SEWER PLAN**

## 2.10 PUBLIC SERVICES

### POLICE AND FIRE

The City of Menifee contracts all law enforcement and fire protection services through the Riverside County Sheriff's Department and the Riverside County Fire Department, respectively. The closest police station to the site is located approximately one mile southwest of the site in Sun City. There are four fire stations in the City and each station has a paramedic engine company.

## 2.11 UTILITIES

### NATURAL GAS

Southern California Gas Company is the main provider for natural gas in this area. Existing gas line exists on Rouse Road and Chambers Avenue and will be the proposed point of connections for the SP. A loop gas system will be installed and Southern California Gas facilities will be placed underground.

### ELECTRICITY

Southern California Edison (SCE) is the main power provider for the SP. Based on current conditions, SCE will provide a main backbone feeder system through proposed Sherman Road from existing SCE facilities on Rouse Road and will tie into Chambers Avenue. All proposed electrical facilities will be underground power distribution system.

### COMMUNICATION

Frontier communications will be the main telephone and fiber provider for the SP. Based on current conditions, Frontier will provide a main backbone feeder (fiber) system through proposed Sherman Road from existing Frontier facilities on Rouse Road. All proposed Frontier facilities will be an underground system

## 2.12 SCHOOLS

Future residents of the Fleming Ranch development would be served by the Menifee Union School District (Menifee USD) for grades K-8 and by the Perris Union High School District (PUHSD) (grades 9-12). Elementary school students would attend Boulder Ridge Elementary School located approximately 1.5 to 2.5 miles to the east of the SP area. Middle school students (7-8) would attend Hans Christensen Middle School located across Chambers Avenue from the SP area. High school students would attend Paloma High School located approximately 4.5 miles south of the SP area.

Development will be required to offset their impact to school districts with upfront development impact fees which are set and collected by each school district in addition to ongoing property taxes. The SP area is located within the boundaries of CFD No.92-1 of the Perris Union High School District.

## 2.13 PHASING PLAN

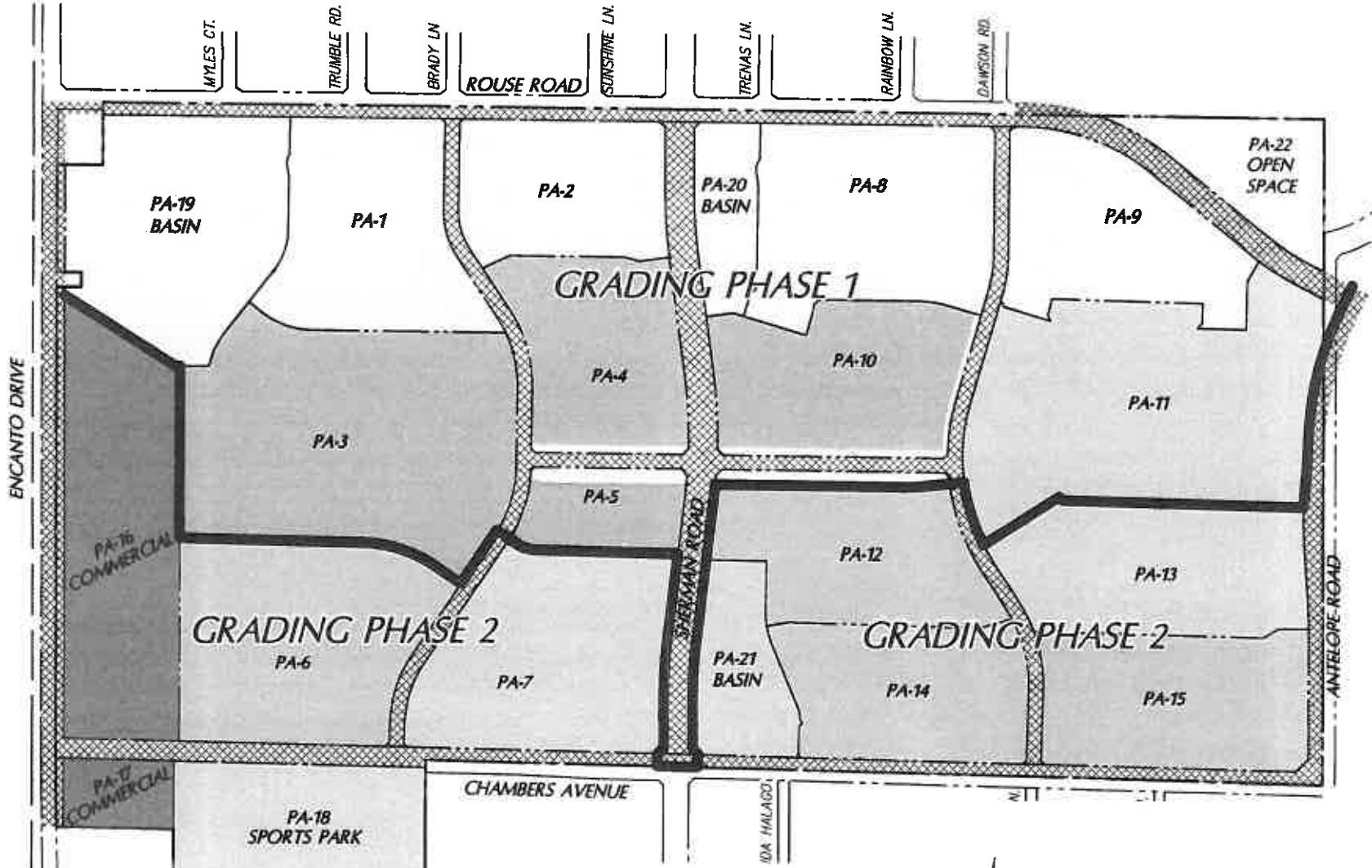
As highlighted on Figure 2.22: Phasing Plan, development and implementation of the Fleming Ranch Specific Plan shall occur in five primary phases. These phases effectively divide the SP area into five areas, primarily utilizing major roadways as phasing boundaries.

The primary objectives for the Fleming Ranch Conceptual Phasing Plan are listed as follows:

- ❖ To ensure orderly and logical development of the Fleming Ranch;
- ❖ To confirm that adequate roadways and infrastructure exist as each phase develops;
- ❖ To validate that residents within a particular phase have sufficient open space and recreational amenities.

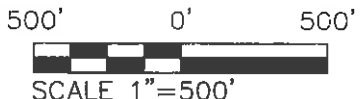
# SECTION 2

## COMMUNITY DEVELOPMENT PLAN



Home Sales Phasing

PHASE	START DATE	LOT COUNT	ABSORPTION RATE	FINISH DATE
1	9/15/20	251	12.5/MO	5/15/22
2	5/15/22	263	12.5/MO	2/15/24
3	2/15/24	284	12.5/MO	1/15/26
4	1/15/26	273	12.5/MO	11/15/27
5	1/15/26	-	-	11/15/27
<span style="display: inline-block; width: 15px; height: 10px; border: 1px solid black; background: repeating-linear-gradient(45deg, transparent, transparent 2px, black 2px, black 4px);"></span> INFRASTRUCTURE IMPROVEMENTS				



**FIGURE 2.22: PHASING PLAN**

It is anticipated that construction of the Fleming Ranch community will be initiated as soon as June 2019 and would be phased based on market demand. The grading of the northern site, Rouse Road frontage improvements and the internal collectors will be constructed in the initial phase of development. Off-site improvements will occur as defined in the Tentative Map application or as the City determines as part of City-wide infrastructure improvements.

It is anticipated that the backbone infrastructure within the Fleming Ranch boundary will be installed in three or more phases. These improvements include rough grading, basin construction, storm drain, water, sewer, dry utilities and street improvements.

The remaining residential internal street improvements, and sports park development would be constructed in phases as shown on Figure 2.22: Phasing Plan. It is expected that construction would occur over an approximate seven-year time frame.

Home construction will include multiple phases within each neighborhood. Starts will be based on sales of homes in the previous phase and market conditions, with the final number of phases to be determined accordingly. The number of phases and number of units in each phase may be altered from time to time, subject to City review and approval and consistency with the Specific Plan and any other City entitlement requirements, including the Fleming Ranch Tentative Maps.



# SECTION 3



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## DEVELOPMENT STANDARDS

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The primary implementation guidance tool for Fleming Ranch is this SP, which establishes the character of the development through the definition of permitted land uses, required infrastructure, development regulations and design guidelines. The standards and regulations contained in this Section, and the Design Guidelines contained in Section 4 provide the framework upon which all subsequent planning and implementation decisions are based, and criteria for determining consistency of site specific design with the SP objectives. It is the purpose of this Section to serve as the development regulations for Fleming Ranch.

### **3.1 SP-WIDE DEVELOPMENT STANDARDS**

#### **3.1.1 MARCH AIR RESERVE BASE COMPATIBILITY**

Disclosures that the property is located within the Airport Influence Area of March Air Reserve Base, as shown on Figure 1.9 Airport Land Use Compatibility Map, shall be provided to new property owners. The Business and Professions Code Section 11010 and Civil Code Sections 1102.6, 1103.4, and 1353 require as part of residential real estate transactions that information be disclosed regarding whether the property is situated within an Airport Influence Area. The Business and Professions Code applies the disclosure requirement to the sale or lease of newly subdivided lands and condominium conversions and to the sale of certain existing residential property. The Civil Code applies the disclosure requirement to existing residential property transfers only when certain natural conditions (earthquake, fire, or flood hazards) warrant disclosure. State Law provides the following disclosure language:

**NOTICE OF AIRPORT IN VICINITY:** This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you.

In addition to the preceding real estate transfer disclosure requirements, a deed notice shall be recorded for each parcel associated with any discretionary land use action affecting property within an Airport Influence Area.

#### **3.1.2 MAXIMUM DEVELOPMENT**

The Maximum development shall not exceed the total unit count and square footage shown on Table 2-1: Land Use Summary.

- ❖ Each Village contains a target number of dwelling units based on adjusted gross density. During the site plan and tentative tract map stage of the development process, the final number of dwelling units for a particular planning area may differ from those identified in the Specific Plan, so long as the density falls within the range specified by the land use designation. Furthermore, the actual amount of units may be less than, but shall not be more than the noted number of dwelling units for each Village as illustrated on the Land Use Plan (Figure 2.2: Land Use Plan).

#### **3.1.3 MAINTENANCE**

Common areas identified in the SP shall be owned and maintained as follows:

- ❖ A permanent master maintenance organization shall be established for the SP area, to assume ownership and maintenance responsibility for all common recreation, open space, circulation systems and landscaped areas. The organization may be public or private. A merger with an area-wide or regional organization will satisfy this standard provided that such organization is legally and financially capable of assuming the responsibilities for ownership and maintenance. If the organization is a private association, then neighborhood associations may be established for each residential development, as needed, and such associations may assume ownership and maintenance responsibilities for neighborhood common areas.

- ❖ Unless otherwise provided for in these standards, common areas shall be conveyed to the maintenance organization as implementing development is approved or any subdivision is recorded.
- ❖ The maintenance organization shall be determined prior to or concurrent with recoordination of any final subdivision map.

### **3.1.4 WATER CONSERVATION**

Landscape and irrigation shall comply with City of Menifee, Landscape Water Use Efficiency Requirements, Ordinance No. 2009-61; Parks, Landscaping, and Tree Preservation, Ordinance No. 2015-167; and State of California AB 1881, the Water Conservation in Landscaping Act (December 2015).

## **3.2 MEDIUM DENSITY RESIDENTIAL (MDR)**

Medium Density Residential (MDR) land uses are proposed for the two Villages for a total of 1,080 homes at an average density of 4.9 dwelling units/acres as shown in Figure 2.2: Land Use Plan. This maximum number of homes shall not be exceeded. The envisioned housing types would be conventional single-family detached homes with attached garages. The homes will have a variety of floor plans and architectural elevations.

As illustrated in Figure 2.1: Conceptual Development Plan, a range of lots sizes is permitted. The five minimum lot sizes proposed are:

- ❖ 5,000 square foot minimum,
- ❖ 5,500 square foot minimum,
- ❖ 6,000 square foot minimum,
- ❖ 6,500 square foot minimum, and
- ❖ 7,000 square foot minimum.

As previously discussed, the conceptual development plan depicts minimum lot sizes varying between 5,000, 5,500, 6,000, 6,500 and 7,000 square feet, the “average” lot size within each lot size is actually much larger.

Final alignments of streets and the placement of lots will be determined during the tentative tract map process. The underlying land use classification remains be Medium Density Residential and is subject to the standards in this subsection.

### **3.2.1 PRINCIPLE PERMITTED USES**

Principle permitted uses include those listed below when developed in compliance with the purpose and intent of this SP.

- ❖ One-family dwellings,
- ❖ Temporary real estate tract offices located within a subdivision to be used for and during the original sale of the subdivision, and
- ❖ Any use that is not specifically listed herein may be considered a principle permitted use or a conditionally permitted use provided that the Community Development Director or designee (Director) finds that the proposed use is substantially the same in character and intensity as those listed in this SP.

### **3.2.2 ACCESSORY PERMITTED USES**

Accessory permitted uses in the MDR area include:

- ❖ Utility facilities,
- ❖ Private recreation facilities,
- ❖ Recreation centers,
- ❖ Swimming pools and spas,
- ❖ Tot lots,
- ❖ Landscaped common areas, and
- ❖ Other accessory uses as determined by the Director to be substantially compatible with a principle permitted residential use.

### **3.2.3 RESIDENTIAL DEVELOPMENT STANDARDS**

The development standards for Medium Density Residential (MDR) designated areas are listed in Table 3.1: Residential Development Regulations.

### **3.2.4 PLOTTING**

All applications for a Plot Plan, CUP, or PUP shall be required to have a minimum number of different floor plans, different front elevations, and different rear elevations for each application as identified below:

- ❖ Neighborhoods with less than 100 units: There shall be three floor plans and three elevations for each floor plan.
- ❖ Neighborhoods with 100 or more units: There shall be four floor plans and four elevations for each floor plan.
- ❖ There shall be three different color schemes per elevation.
- ❖ Reverse floor plans should be included where possible to add variety to the street scene.

**TABLE 3.1: RESIDENTIAL DEVELOPMENT REGULATIONS**

STANDARD	5000 SF LOTS
<b>LOT DIMENSIONS</b>	
Minimum lot size	5,000 SF
Minimum width	50'
Minimum depth	100'
Minimum frontage <sup>1</sup>	45'
Minimum frontage on lots fronting knuckles or cul-de-sacs	35'
Flag lots <sup>2</sup>	20'
<b>FRONT SETBACK (FROM PROPERTY LINE)</b>	
To living area	15'
To a front entry garage	20'
To a side-in garage	10'
To a porch, patio cover or 2nd-story deck	10'
<b>SIDE SETBACK (FROM PROPERTY LINE)</b>	
Minimum interior side yard	5'
Minimum corner side yard <sup>3</sup>	10'
<b>REAR SETBACK (FROM PROPERTY LINE)</b>	
To living area	10'
To California Room <sup>4</sup>	10'
To garage, patio cover, 2nd-story deck, trellis or support structure	5'
To a pool and/or pool equipment	5'
<b>WALLS, FENCES AND BARRIERS</b>	
Maximum height within front yard setback	3'
Maximum height at interior or rear property line	9' on top of a 3' retaining wall
<b>GENERAL REGULATIONS</b>	
Maximum structural height	40'
Maximum lot coverage	65% for 1-story & 60% for 2-story
Yard encroachments (Uninhabitable architectural features than extend beyond the building face including eaves, chimneys, bay windows, stairways, and other architectural detailing)	2'

**TABLE 3.1: RESIDENTIAL DEVELOPMENT REGULATIONS**

STANDARD	5000 SF LOTS
<b>AIR CONDITIONING</b>	
Air conditioning (AC) units	ACs may encroach into the side yard setback, but must provide a 3' clear flat area between the AC and the property line fence or wall. AC units shall be placed in the non-gated side yard when applicable.
<b>MULTI-GENERATIONAL SUITE STANDARDS</b>	
General standards	Multi-generational suites are defined as living areas connected to the home structurally and through an entrance from the main home, although a separate exterior door is allowable. Multi-generational suites may include a sleeping area, sitting area, kitchenette and closet.
Zoning requirements	Multi-generational suites are permitted on all lots, provided that it meets all of the zoning requirements described above.
Additional standards	<ul style="list-style-type: none"> <li>• Single gas, water and electrical meters are required; Conformance with the City's parking standards are required (garage conversions prohibited);</li> <li>• Complete kitchen facilities containing a stove, range or oven are prohibited. Alternatively a kitchenette may be allowed. A kitchenette may contain a sink, refrigerator and an electrical outlet which may be used for a microwave oven. No 22V outlets for a range or oven shall be provided;</li> <li>• Incorporate Universal Design into multi-generational floor plan in order to accommodate transitional aging of seniors and have any multi-generational floor plan certified by a Universal Design Certified Professional; and</li> <li>• Unit must be integrated (connected to the main unit).</li> </ul>

<sup>1</sup>The length of the defined front lot line measured at the street right-of-way.

<sup>2</sup>Flag lots shall meet all lot requirements except street frontage may have an access strip not less than 20' wide. In instances where a driveway exceeds 150' a turnaround area approved by the fire department will be required.

<sup>3</sup>In instances where a corner lot is located adjacent to a landscape area, and the landscape area separates the residential lot from the right-of-way, then the landscape area may be counted towards the setback requirement.

<sup>4</sup> A California Room is defined as a built-in covered patio incorporated in to the house design and roof line. California Rooms are located at the rear or side of the home are open to the outdoors, and may be enclosed on up to three sides

### 3.3 COMMERCIAL

The commercial land use within the SP, is similar to the Economic Development Corridor zoning district and similar uses and setbacks included in this SP.

#### 3.3.1 PERMITTED USES

Administrative design review approvals and prohibited uses are shown in Table 3.2: Commercial Permitted Uses. Uses not listed in the table below are either not permitted as a part of this SP or require separate processing pursuant to the Menifee Municipal Code (MMC).

**TABLE 3.2: COMMERCIAL PERMITTED USES**

USE	NOTES	
ADR = Administrative Design Review; P = Permitted Uses; C = Conditional Uses; - =Expressly Prohibited Uses		
<b>Commercial/Office</b>		
<b>Alcohol</b>		
Alcoholic beverage sales:		
• On-site consumption;	C	
• Off-site consumption-Primary product of retail outlet;	C	
• Off-site consumption-Accessory product of retail outlet;	P	
• Breweries, distilleries, and wine making facilities with on-site tasting room and sales for off-site consumption.	P	
<b>Automotive Related Sales and Services</b>		
Automotive parts and accessory stores.	P	
Automotive services/repairs (e.g., tune-ups, emission tests, brakes, tires, batteries, electrical, etc.)	P	Activity must be conducted entirely within an enclosed building. Incidental, screened, outdoor storage is prohibited.
Automotive repairs - major (e.g., engine and transmission repair/rebuild, etc.)	C	Services must take place entirely within an enclosed building.
Vehicle storage and impoundment within an enclosed building.	-	
Boat sales, new and used.	C	
Body, paint and upholstery shops.	C	
Car wash - full or self-service.	C	
Gas station with/without a mini-market.	-	
Motorcycle sales/service.	P	Services conducted entirely within an enclosed building.

**TABLE 3.2: COMMERCIAL PERMITTED USES**

USE	NOTES
ADR = Administrative Design Review; P = Permitted Uses; C = Conditional Uses; - =Expressly Prohibited Uses	
Motor vehicle sales, new and used (Including repairs associated with sales).	C
Motor vehicle, trailer or boat storage. • Indoor • Outdoor	C -
Parking:	
• Commercial lot/garage.	C
	Parking stalls must be within a building or screened with a combination of walls and landscaping. Alternative screening structure/device may be approved by the Planning Commission.
Towing services (with tow truck parking - no auto storage).	-
Trailer and mobile home sales and rental.	-
Rental:	
• Automobiles;	C
• Truck, trailer and van;	C
• Equipment;	-
• Construction equipment.	-
<b>Day Care Facilities</b>	
Commercial - more than 14 children.	C
Large family home day care (8-14 children).	-
Small family home day care (fewer than 8 children).	-

**TABLE 3.2: COMMERCIAL PERMITTED USES**

USE	NOTES
ADR = Administrative Design Review; P = Permitted Uses; C = Conditional Uses; - =Expressly Prohibited Uses	
<b>Eating/Drinking Places and Food Services</b>	
Banquet facilities.	C
Bar/cocktail lounge.	C
Catering establishment (Includes truck parking).	C
Coffeehouse: • No Live Entertainment; • With live entertainment.	P C
Fast food/quick service: • With drive-through; • Without drive-through.	C P
Food Truck.	See Chapter 9.98 "Mobile Food Vendors" of the MMC.
Restaurants: • Sit down/full service; • Outdoor dining.	P P
Restaurants with breweries, distilleries and wine making facilities with sales for on-site and off-site consumption.	P
<b>Entertainment and Recreation</b>	
Adult-oriented business.	-
Amusement park (including multiple activities such as simulated flying, racing, dragster, slick track, skate park, etc.): • Within a building; • Outside.	C -
Athletic fields	-

**TABLE 3.2: COMMERCIAL PERMITTED USES**

USE	NOTES
ADR = Administrative Design Review; P = Permitted Uses; C = Conditional Uses; - =Expressly Prohibited Uses	
Auditoriums and other public/private assembly facilities (including live entertainment): • Indoor; • Outdoor.	A CUP is required for the sale/provision of alcohol. C -
Batting cages: • Indoor; • Outdoor.	P -
Motocross Facilities: • Bicycle (BMX) course; • Off-road mini-bike course.	- -
Billiard parlor/pool hall.	C A CUP is required for the sale/provision of alcohol.
Bowling center.	C A CUP is required for the sale/provision of alcohol.
Cybercafe.	P
Go-cart track: • Indoor; • Outdoor.	C -
Golf course - Miniature.	-
Golf course - Full Course.	-
Golf driving range (not in association with full scale course).	-
Health club/gymnasium; Indoor.	P
Private clubs and lodges.	C
Movie theater.	C A CUP is required for the sale/provision of alcohol.
Recording and sound studios.	P
Simulated shooting games: • Indoor (laser tag, etc.); • Outdoor (paintball, etc.).	C - Freeway frontage allowed for indoor facilities.

**TABLE 3.2: COMMERCIAL PERMITTED USES**

USE	NOTES
ADR = Administrative Design Review; P = Permitted Uses; C = Conditional Uses; - =Expressly Prohibited Uses	
Shooting range; Indoor.	C
Skate park: • Commercial; • Private.	- -
Skating rink (ice/roller).	C
Smoking Lounge.	C
Stadium/sport arena.	-
Tennis/swim club; Outdoor (not assoc. with larger sporting facility).	-
Video/electronic/computer game arcade.	P Services must be conducted within an enclosed building.
<b>Lodging</b>	
Hotel.	P
Motel.	-
Bed and Breakfast Inn.	-
Recreational vehicle parks and campgrounds, not exceeding a density over (10) units to the acre.	-
<b>Retail/Wholesale Sales</b>	
Adult book store.	-
Antique shop.	P
Art galleries and art supply store.	P
Auction facility (non vehicle): • Indoor (includes storage); • Outdoor.	P - Temporary or one-time event, see Chapter 9.48 "Temporary Uses" of the MMC.
Auction facility (vehicle).	-
Bakeries: • Retail only; • Wholesale.	P - -
Bicycle shop, sales and repair.	P



**TABLE 3.2: COMMERCIAL PERMITTED USES**

USE	NOTES	
ADR = Administrative Design Review; P = Permitted Uses; C = Conditional Uses; - =Expressly Prohibited Uses		
Hardware store • Within enclosed building; • With outdoor storage/sales.	P C	Outdoor nursery/plant sales up to 25% of building floor area. 26% nursery/plant sales, see "Agriculture."
Camera and photographic supplies.	P	
Farmer's market, open air market, craft or job fair.	C	See Chapter 9.72 "Farmer's Market, Open Air Market, Craft or Job Fair" of the MMC.
Candy, confectionery.	P	
Computer, radio, television, and small electrical appliance shop (with incidental repair).	P	No outdoor storage.
Drug store.	P	A CUP is required for the sale/provision of alcohol.
Feed store.	-	
Florist shop.	P	
Furniture and home furnishing store.	P	
Department store.	C	
Discount/Variety store (new items only).	P	CUP required to sell alcohol.
Gift and/or souvenir store.	P	
Grocery store.	P	CUP required to sell alcohol if grocery store is under 20,000 square feet in retail sales area.
Guns and ammunition store.	-	
Hobby, toy and game store.	P	
Household appliance store.	P	

**TABLE 3.2: COMMERCIAL PERMITTED USES**

USE	NOTES	
ADR = Administrative Design Review; P = Permitted Uses; C = Conditional Uses; - =Expressly Prohibited Uses		
Multi-tenant retail shopping center.	P	New merchandise, collectables and antiques only (Not a "Swap Meet.")
Kiosk, non-vehicular.	P	
Music, record and video sales.	P	
Office supply and/or stationary store.	P	
Pawnbroker/pawnshop facilities.	-	
Pet and pet supply store.	P	
Secondhand and thrift store.	C	No outdoor display or storage.
Sporting goods store.	P	Gun/ammunition sales, see "Gun and ammunition store" above.
Swap meets: • Indoor; • Outdoor.	C -	May not be located where visible from the freeway.
Warehouse/club store: • Stand alone facility under 50,000 sq. ft.;	P	
• Stand alone facility 50,000 sq. ft. or larger.	C	
Warehouse/club store: • Within a complex/center regardless of size.	C	
Wholesale stores and distributors.	C	Services must be conducted entirely within an enclosed building.

**TABLE 3.2: COMMERCIAL PERMITTED USES**

USE	NOTES	
ADR = Administrative Design Review; P = Permitted Uses; C = Conditional Uses; - =Expressly Prohibited Uses		
<b>Services</b>		
Ambulance service.	C	
Blueprinting.	P	
Facilities where charitable donations are: • Received and/or processed and/or sold.	C	Stand alone donation boxes are prohibited.
Exterminating.	C	Offices only, no hazardous materials storage.
Small equipment rental (lawn mowers, cement mixer, mobile pumps, trailers, pick-up trucks, etc.)	-	
Large equipment rental (trucks with beds over 18 feet in length, eighteen plus (18+) wheelers, bulldozers, construction lifts and cranes).	-	
General office uses.	P	
Photocopying/duplicating.	P	
Photographic studio.	P	
Physical Therapy.	P	
Financial services: • Banks/credit unions; • Check cashing/payday advance; • Mortgage/lending.	P C P	

**TABLE 3.2: COMMERCIAL PERMITTED USES**

USE	NOTES	
ADR = Administrative Design Review; P = Permitted Uses; C = Conditional Uses; - =Expressly Prohibited Uses		
Personal services: • Barber shop; • Beauty or nail salon; • Dry cleaner; • Massage parlor; • Massage therapy office; • Pet grooming; • Tailor; • Tattoo/body piercing.	P P P - - C P -	
Cemeteries and mausoleums.	-	
Funeral parlor, mortuary with crematorium.	-	
Laundry-Commercial: • 2,500 sq. ft. or less; • More than 2,500 sq. ft. • Self-serve.	P - P	
Medical marijuana dispensary or facility.	-	
Taxidermy.	-	
Fortune telling.	-	
Kennel, public or private. Indoor only.	C	See Chapter 9.64 "Animal Regulations" of the MMC.
Recycling collection facilities: • 500 or less square feet; • More than 500 square feet.	- -	

**TABLE 3.2: COMMERCIAL PERMITTED USES**

USE	NOTES
ADR = Administrative Design Review; P = Permitted Uses; C = Conditional Uses; - =Expressly Prohibited Uses	
<b>Repair service:</b> <ul style="list-style-type: none"> <li>• Computer, home electronic and small appliances;</li> <li>• Electrical equipment, industrial;</li> <li>• Furniture refinishing;</li> <li>• Furniture re-upholstery</li> <li>• Home appliances; Jewelry/ watches/clocks; Lawnmower/ garden equipment;</li> <li>• Locksmith/key shop;</li> <li>• Shoe repair.</li> </ul>	P P P P P P
Services must be conducted entirely within an enclosed building, outdoor storage is not allowed.	
<b>Agricultural</b>	
<b>Plant nurseries:</b> <ul style="list-style-type: none"> <li>• Retail Sales, indoor or outdoor;</li> <li>• Cultivation of plants, indoor or outdoor.</li> </ul>	C C
Excludes materials yards.	
Marijuana Cultivation.	-
<b>Institutional Uses</b>	
<b>Animal hospital:</b> <ul style="list-style-type: none"> <li>• Small animals</li> <li>• Large animals.</li> </ul>	P -
No exterior kennels, pens or similar enclosures.	
Assisted living/community care facilities.	C
Churches, synagogues, temples and other religious facilities.	C
Community center.	P
Congregate care facility.	C
Convalescent hospital/care facility.	P
Governmental facility.	P
Hospital.	C

**TABLE 3.2: COMMERCIAL PERMITTED USES**

USE	NOTES
ADR = Administrative Design Review; P = Permitted Uses; C = Conditional Uses; - =Expressly Prohibited Uses	
Research and development laboratory.	P
Services must be conducted completely within an enclosed building.	
<b>School:</b> <ul style="list-style-type: none"> <li>• Private;</li> <li>• Public;</li> <li>• Vocational/trade school;</li> <li>• University/college.</li> </ul>	C P C C
Public schools are regulated by the State.	
<b>Storage</b>	
Contractor storage yard (no retail sales).	-
Public Self-Storage.	-
Private materials yard (outdoor storage of privately owned materials not associated with a commercial activity).	-
Lumber yard.	-
Material storage yard (wholesale sales).	-
Parcel delivery service.	P
Warehouses and storage buildings.	C
Services must be conducted entirely within an enclosed building.	

**TABLE 3.2: COMMERCIAL PERMITTED USES**

USE	NOTES	
ADR = Administrative Design Review; P = Permitted Uses; C = Conditional Uses; - =Expressly Prohibited Uses		
<b>Transportation, Communication and Utilities</b>		
Communication facilities; • Cell towers.	C	See Chapter 9.100 "Wireless Communications" and 9.102 "Antenna and Satellite Regulations" of the MMC.
Transportation facilities: • Bus stops; • Truck stops; • Heliport; • Airport; • Carpool facility/lot;	P - - - P	
Utility: • Service uses and structures; • Utility offices.	P P	Regulated by the PUC; Commercial solar fields or wind farms are expressly prohibited.

**TABLE 3.2: COMMERCIAL PERMITTED USES**

USE	NOTES	
ADR = Administrative Design Review; P = Permitted Uses; C = Conditional Uses; - =Expressly Prohibited Uses		
<b>Industrial Uses</b>		
Light Manufacturing of food, lumber, wood, and paper products: • Grain and bakery products; • Sugar and confectionary products; • Nonalcoholic beverages; • Ice; • Manufacture of furniture and fixtures including cabinets, partitions, and similar items; • Mulch production; • Printing and publishing or newspapers, periodicals, books, forms, cards, and similar items; • Binding of books and other publications; • Rendering (no on-site slaughtering).	- - - - - - - -	
Textile and leather products: • Wearing apparel and accessory products; • Manufacture of handbags luggage, footwear, and other personal leather goods.	- -	
Chemical and glass products: • Pharmaceutical manufacture; • Glassblowing, pressing, cutting and other glassware products.	- -	

**TABLE 3.2: COMMERCIAL PERMITTED USES**

USE	NOTES	
ADR = Administrative Design Review; P = Permitted Uses; C = Conditional Uses; - =Expressly Prohibited Uses		
Metal, machinery, and electrical products:	-	
• Jewelry manufacture and repair; Manufacture, assembly, testing and repair of components, devices equipment and systems of an electrical, electronic, or electro mechanical nature, such as, but not limited to:		
a) Television and radio equipment and systems;		
b) Phonographs and audio units;		
c) Metering instruments, equipment and systems;		
d) Radar, infrared and ultraviolet equipment and systems;		
e) Coils, tubes, semiconductors and similar components;		
f) Scientific and mechanical instruments;		
g) Data processing equipment and systems;		
h) Communications, navigation control, transmission and reception equipment, control equipment and systems, guidance equipment and systems;		
j) Musical and recording equipment.		

**TABLE 3.2: COMMERCIAL PERMITTED USES**

USE	NOTES	
ADR = Administrative Design Review; P = Permitted Uses; C = Conditional Uses; - =Expressly Prohibited Uses		
• Office and computing machine manufacturing.	-	
• Control devices and gauges.	-	
• Manufacture of light fixtures and supplies.	-	
Engineering and scientific instruments:		
• Manufacture and repair of engineering, scientific, and medical instrumentation including but not limited to:		
a) Measuring devices, watches, clocks, and related items;	C	
b) Optical goods;	C	
c) Medical, and dental instruments;	C	
d) Engineering, survey, and drafting instruments;	C	
e) Photographic equipment.	C	
Solar power generating facilities.	-	
Recycling processing facilities conducted on an industrial scale.	-	
<b>Accessory Uses</b>		
Attached, unenclosed patio roofs, decks, porches, awnings, canopies and other similar shading devices and structures.	P	

### 3.3.2 COMMERCIAL DEVELOPMENT STANDARDS

The development standards for the Commercial area are listed on Table 3.3: Commercial Development Standards. Enhanced dense landscape is required adjacent to Encanto Drive and the residential zones.

**TABLE 3.3: COMMERCIAL DEVELOPMENT STANDARDS**

ELEMENT	STANDARD
<b>Minimum Lot Area</b>	None.
<b>Front Setback</b>	15' densely landscaped.
<b>Street Side Setback</b>	10' landscaped.
<b>Rear Setbacks</b>	20' densely landscaped.
<b>Building Height</b>	45'; Structural features which are not an essential and/or integral part of the structure such as chimneys or similar features, as well as flag poles may exceed height limits by up to 15'.
<b>Parking</b>	As required by Section 18.12 of City of Menifee Zoning Code.
<b>Walls</b>	Prior to occupancy a six foot high solid masonry wall shall be constructed on each property line that adjoins any parcel designated for residential land use. A view fence shall be permitted when adjoining open space uses.
<b>Landscaping</b>	A minimum of 10% of the site for parcels less than one acre shall be landscaped and irrigated and 5% of the total lot area for parcels exceeding one acre, excluding the portion of the lot contained within the required front setback area.
<b>Trash Collection Areas</b>	Trash collection areas shall be screened by landscaping or architectural features in such a manner as not to be visible from a public street or from any adjacent residential or open space areas.
<b>Outside Storage Areas</b>	Prohibited.
<b>Mechanical Equipment</b>	All roof mounted mechanical equipment shall be screened from the ground elevation view to a minimum sight distance of 1,320'.
<b>Lighting</b>	All lighting fixtures, including spot lights, electrical reflectors and other means of illumination for signs, structures, landscaping, parking, loading, unloading and similar areas, shall be focused, directed, and arranged to prevent glare or direct illumination on streets or adjoining property.
<b>Building Articulation</b>	Facades shall be modulated to create visual and architectural interest. Architectural elements including bays and recesses, balconies and terraces, inset windows that allow for the expression of wall thickness, patterns of shade and shadow at facades, changes of material and color, use of architectural details such as horizontal and vertical banding, cornices, door and window surrounds, and use of high-quality materials, such as smooth finished stucco, brick and stone are encouraged.

\*Setback areas may be used for driveways, parking and landscaping.

### **3.4 PARK AND RECREATION**

As shown in Figure 2.2: Land Use Plan, Fleming Ranch contains 17.7 acres of park land.

#### **3.4.1 PRINCIPLE PERMITTED USES**

The principle permitted uses in the Parks and Recreation zone include those listed below when developed in compliance with the purpose and intent of this Specific Plan:

- ❖ Public or private parks;
- ❖ Public playgrounds;
- ❖ Flood control basins, detention basins, retention basins and related facilities; and
- ❖ Athletic fields.

#### **3.4.2 ACCESSORY PERMITTED USES**

Accessory permitted uses in the Parks and Recreation zone include parking lots, only for the above permitted uses:

- ❖ Utility facilities;
- ❖ Recreation facilities;
- ❖ Trails;
- ❖ Shade structures; and
- ❖ Other accessory uses as determined by the Director to be substantially compatible with a principle permitted open space recreation/park use.

#### **3.4.3 REQUIRED AMENITIES—SPORTS PARK**

At a minimum, the Sports Park shall include the following amenities:

- ❖ Athletic field(s);
- ❖ Play area(s);

- ❖ Walkway(s);
- ❖ On-site parking;
- ❖ Shade tree plantings and rolling turf areas;
- ❖ Restrooms; and
- ❖ Field lighting.

### **3.5 OPEN SPACE CONSERVATION**

As shown in Figure 2.2: Land Use Plan, approximately 6.3 acres are designated as Open Space Conservation. The Open Space Conservation (OS-C) area is not considered suitable for development and will therefore remain as unimproved land.

#### **3.5.1 PRINCIPLE PERMITTED USES**

The principle permitted uses in the open space conservation area include those listed below when developed in compliance with the purpose and intent of this Specific Plan:

- ❖ Unrestricted open space; and
- ❖ Utility facilities.

### 3.5.2 ACCESSORY PERMITTED USES

The accessory permitted uses in the Open Space and Conservation area include:

- ❖ Trails, and
- ❖ Other accessory uses as determined by the Planning Director to be substantially compatible with a principle permitted open space conservation use.

### 3.6 OS-W

As shown in Figure 2.2: Land Use Plan, approximately 27.5 areas are designated as OS-W for flood control and water quality. The purpose of this designation is to allow for drainage corridors and water treatment facilities.

#### 3.6.1 PRINCIPLE PERMITTED USES

The principle permitted uses of the Open Space Water zone include:

- ❖ Detention facilities, and
- ❖ Water quality treatment facilities.

#### 3.6.2 ACCESSORY PERMITTED USES

The accessory permitted uses of the OS-W zone include:

- ❖ Off-site parking for existing facilities adjacent to Encanto Drive only, and
- ❖ Landscape that doesn't interfere with drainage facilities.

### 3.7 LIGHTING

All lighting shall comply with the following regulations and provisions:

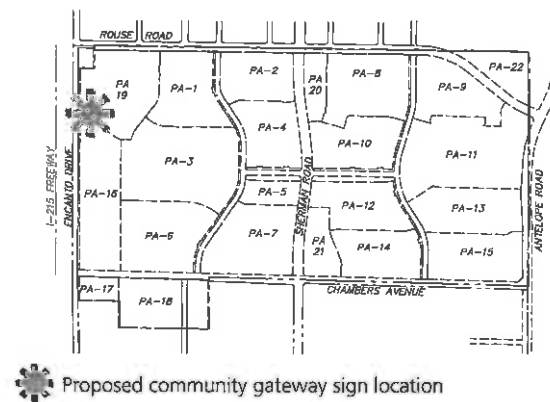
- ❖ All outdoor lighting, including spotlights, floodlights, electrical reflectors and other means of illumination for signs, structures, landscaping,

- ❖ Similar areas shall be focused, directed, and arranged to prevent glare and illumination on streets or adjoining property; and
- ❖ All outdoor lighting shall comply with the requirements of Chapter 6.01: Dark Sky; Light Pollution of the MMC.

### 3.8 COMMUNITY GATEWAY SIGN

One Community Gateway Sign with an electronic message board is allowed within the SP, adjacent to I- 215. This Gateway shall be in compliance with the following:

- ❖ Maximum sign height shall be 45 feet;
- ❖ Maximum sign area shall be 100 square feet;
- ❖ The Electronic Message Board portion of the allowable freestanding sign may not exceed seventy-five (75) percent of the total sign area and must be integrated with the remainder of the sign to form a cohesive design unit; and
- ❖ The Electronic Message Board shall contain a default mechanism that will cause the sign to revert immediately to a black screen if the sign malfunctions.



**FIGURE 3.1: PROPOSED COMMUNITY GATEWAY PLAN**



# SECTION 4



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## DESIGN GUIDELINES

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The Design Guidelines Section provides the design framework for streetscape, landscape, and buildings to convey a cohesive master plan identity. They establish the pattern and intensity of development for Fleming Ranch to ensure a high-quality and aesthetically cohesive environment. While these guidelines establish the quality of the architectural and landscape development for the master plan, they are not intended to prevent alternative designs and/or concepts that are compatible with the overall project theme.

The design guidelines chapter is organized as follows:

- |                                |   |
|--------------------------------|---|
| 4.1 Guiding Principles,        | 4.4 Residential Architectural Requirements, |
| 4.2 Landscape Guidelines,      | 4.5 Residential Architectural Styles, and   |
| 4.3 Residential Site Planning, | 4.6 Non-Residential Guidelines.             |

## 4.1 GUIDING PRINCIPLES

The following guiding principles will positively influence the design of the Fleming Ranch to ensure quality development:

- ❖ Create a community that encourages interaction and evokes a “pride of place” where people want to live;
- ❖ Encourage linkages and connectivity through land use adjacencies, trails, and open space;
- ❖ Create a variety of walkable neighborhoods;
- ❖ Encourage physical, social and economic diversity; and
- ❖ Integrate environmentally responsible practices.

These Design Guidelines are also intended to be flexible and are, therefore, illustrative in nature. As a flexible document, the Guidelines can, over time, accommodate changes in lifestyles, consumer preferences, economic conditions, community desires and the marketplace.

The landscape and architectural guidelines complement each other. Together they combine to form a distinctive master plan offering a high quality, sustainable environment and a sense of identity.

## 4.2 LANDSCAPE DESIGN GUIDELINES

### 4.2.1 COMMUNITY DESIGN THEME

Landscape architectural design plays an important role in establishing the visual identity and character of a community. Consistency in theme and the application of major community-level elements such as entries, collector enhanced paseos, walls and fences, interface/edge conditions, and plant material must be maintained to communicate and strengthen this identity. This subsection establishes guidelines to ensure that a cohesive landscape fabric will be created to unify the community at all levels of development.

Careful consideration has been given to integrate structural and aesthetic elements of Fleming Ranch to create a balanced, aesthetically appealing community. Several identifying design and landscape elements will be incorporated:

- ❖ Stone veneer for monuments and accessory structures,
- ❖ Natural landscaped areas designed with an ecological approach,
- ❖ Native and other climate-appropriate plant material,
- ❖ Natural materials such as stone or wood, or
- ❖ Multiple paving materials such as stone, concrete, decomposed granite and concrete pavers.

Fleming Ranch captures and enhances the unique character of the Menifee area, offering residents an environment where walkability, recreational activity and social interaction are encouraged. A semi-formal design aesthetic has been used to create structure and movement without rigidity. Extreme temperature fluctuations of Menifee have been taken into consideration. The high heat of the summer months makes shade a primary consideration. This SP focuses on these aspects by providing generous landscape setbacks, aesthetically pleasing streetscapes, recreational amenities, public gathering areas, and enhanced paseos and landscaped open spaces. Thematic elements establish and reinforce the design theme. These major thematic elements include:

- ❖ Monumentation,
- ❖ Streetscapes,
- ❖ Parks,
- ❖ Enhanced paseos,
- ❖ Open space,
- ❖ Enhanced landscape areas,
- ❖ Lighting, and
- ❖ Walls and fences.

These elements unite Fleming Ranch under a common design vocabulary. General landscape design guidelines and design criteria for thematic elements are contained in the subsections that follow.

# SECTION 4

## DESIGN GUIDELINES

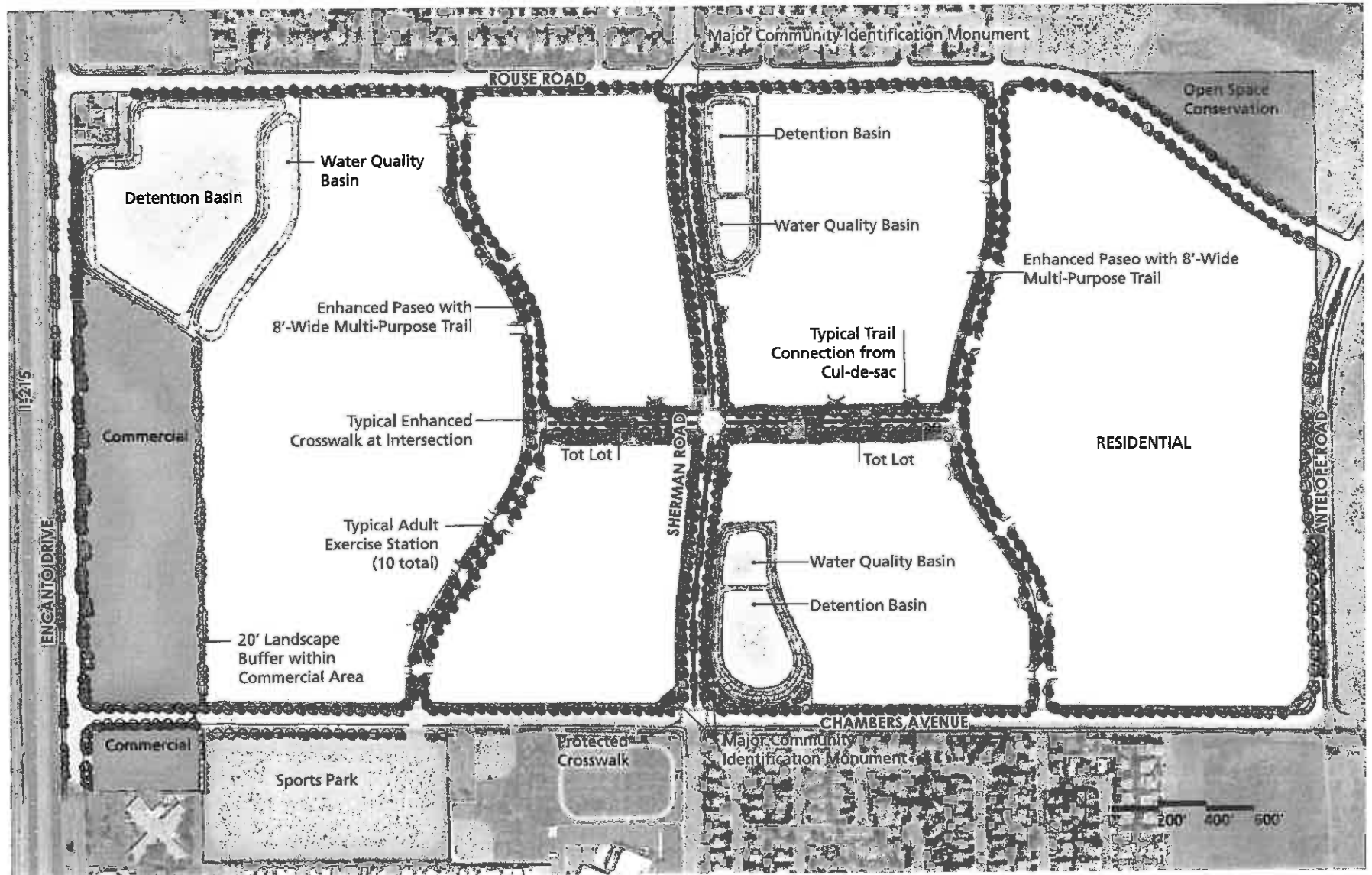
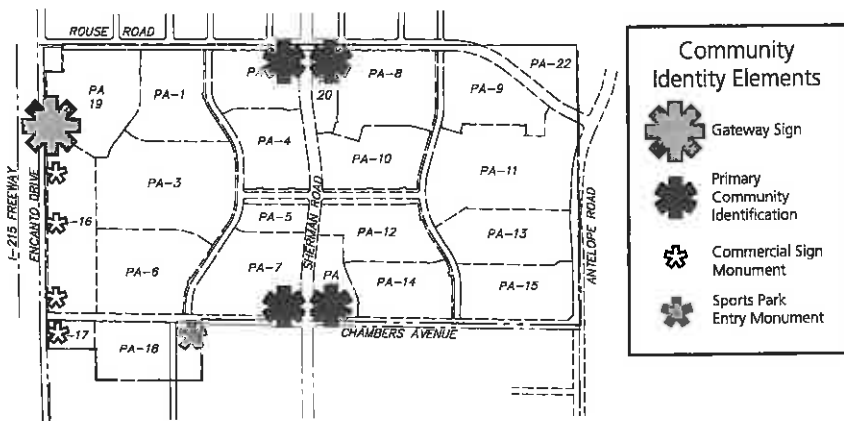


FIGURE 4.1 CONCEPTUAL LANDSCAPE & COMMUNITY IDENTITY PLAN

**4.2.2 COMMUNITY IDENTITY PLAN**

Appropriate community and neighborhood thematic identification is important for the overall Fleming Ranch theme. These elements provide a system for identifying neighborhoods and give directional information to residents and visitors. An overall conceptual identity plan has been provided to convey the vision for thematic elements. Refer to Figure 4.2 Conceptual Community Identity Plan. Entry monument signage informs viewers through decorative typefaces and symbolic graphics that they are entering a master planned community. Project and neighborhood signage should direct those who have entered Fleming Ranch to individual land use components. Monuments should be consistent with the community character but flexible enough to respond to the individual neighborhood contexts. Logos, type styles, color schemes, and architectural features should be consistent throughout the area being identified. Monument signs may vary in size and detail in a manner that reflects their relative importance within the identification hierarchy.



**FIGURE 4.2 CONCEPTUAL COMMUNITY IDENTITY PLAN**

Community identification will be accomplished with the use of placemaking elements including architectural features, thematic landscape treatments, and Fleming Ranch monuments. The final design for all signage and monuments shall be reviewed and approved by the City of Menifee prior to the issuance of building permits for each associated development phase. As shown in the following Figures, the general dimensions and detail of each structure will vary based on the intended use of the identification element.

**GATEWAY SIGN**

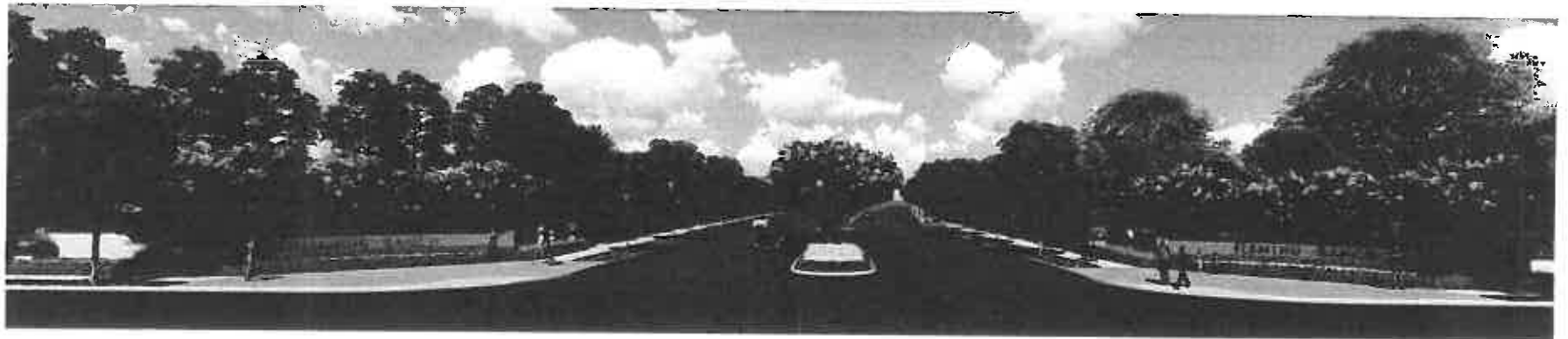
An attractive and unified community appearance not only increases a sense of pride in residents, but creates a positive climate for business and makes a positive impression on visitors. The City of Menifee’s image can be reinforced at carefully placed and well-designed gateways at primary entrances to the city; these gateways can define the boundaries of the City and create a sense of arrival. The SP site has been identified as the location of a Gateway Sign into Menifee are shown on Figure 4.2 Conceptual Community Identity Plan. This Gateway Sign could include a large freeway oriented sign identifying the City of Menifee. The Gateway Sign may include an electronic reader board that the City could manage advertising on it.



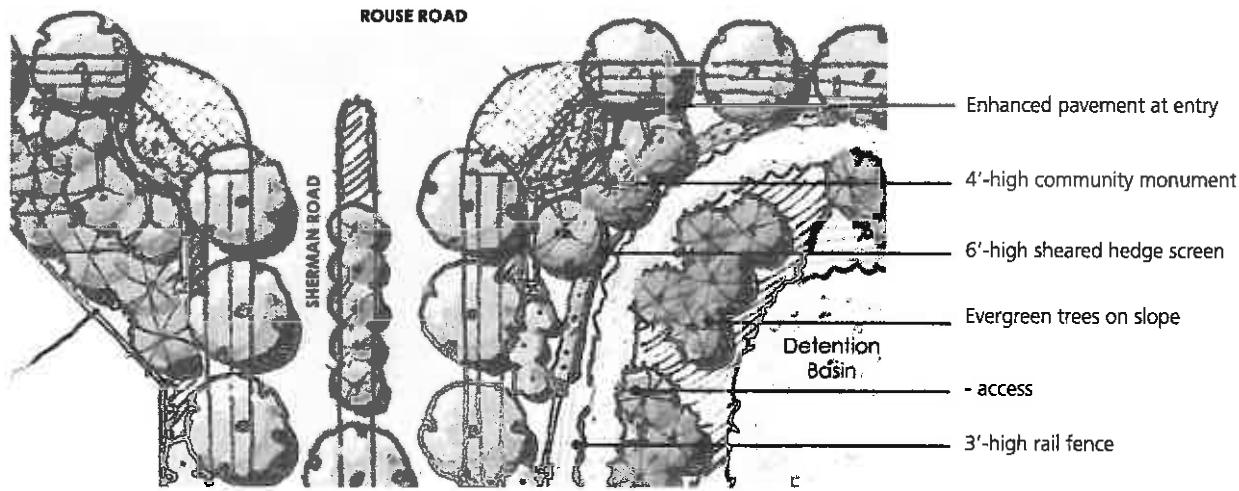
*Gateway Sign Examples*

**PRIMARY COMMUNITY ENTRY MONUMENTS**

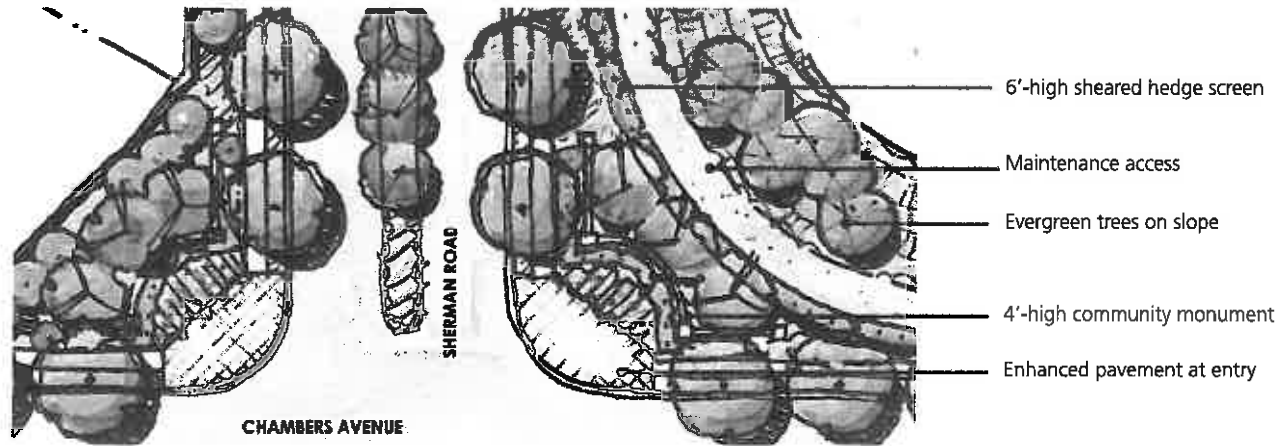
Primary Community Entry Monuments will be used to identify the main community entry points at the intersection of Sherman Road with Chambers Avenue and Rouse Road. The Primary Community Entry Monuments incorporate several design elements including a stone-faced sign wall in front of flowering trees, with decorative planter pots. These monuments create a feeling of cohesion and serve as welcoming elements for residents and visitors entering the community.



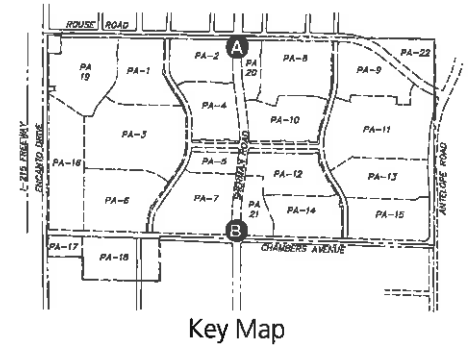
**FIGURE 4.3: SOUTH COMMUNITY ENTRY PERSPECTIVE VIEWS**



**FIGURE 4.4A: NORTH COMMUNITY ENTRY**



**FIGURE 4.4B: SOUTH COMMUNITY ENTRY**



# SECTION 4 DESIGN GUIDELINES

## COMMERCIAL SIGN MONUMENTS

The commercial sign monuments will be located and sized according to each specific use and will generally consist of a stone faced base element, decorative stucco sign panel and a brick cap. These monuments will include a common sign panel and sufficient space for tenant identification and/or building addresses.

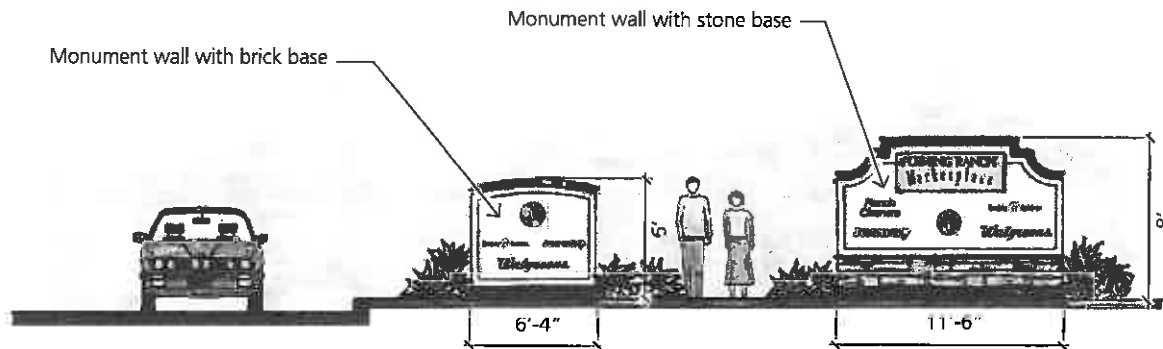


FIGURE 4.5: COMMERCIAL SIGN MONUMENTS

## PARK ENTRY MONUMENT

The Park Entry Monument is located at the Regional Sports Park entrance south of Chambers Avenue.

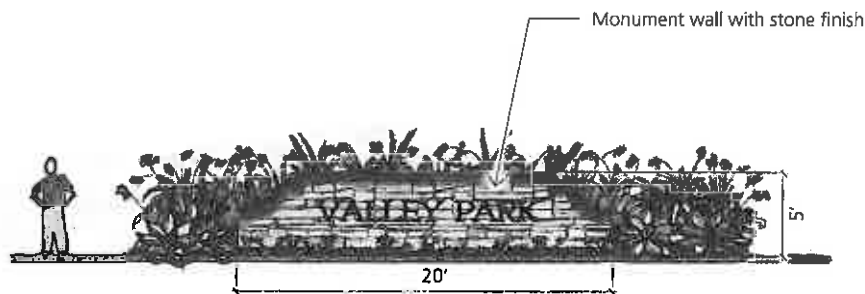
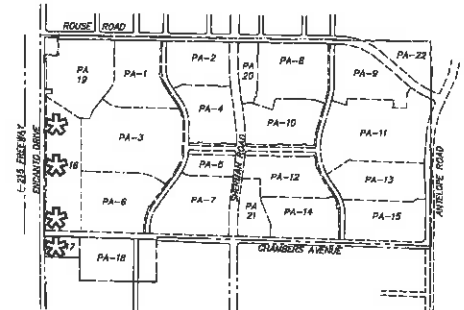
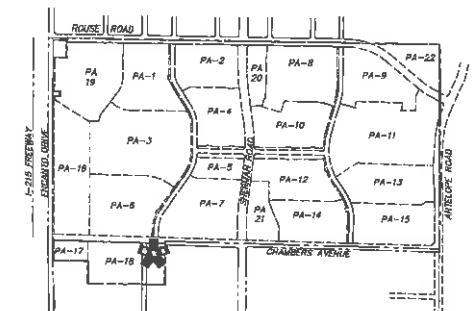


FIGURE 4.6: PARK ENTRY MONUMENT



Key Map



Key Map

### **4.2.3 STREETSAPES**

A hierarchy of streetscapes is proposed within Fleming Ranch and distinctive landscape treatments are to be planned for each neighborhood. Pedestrian-friendly landscape treatments include elements such as thematic street trees, enhanced paseos, entries and landscaped medians as discussed below.

#### **MAJOR AND SECONDARY ROADWAYS**

Along Chambers Avenue, Sherman Road, Encanto Drive, Rouse Road and Antelope Road, the landscape should soften, accent and enhance the perimeter walls, as well as accommodate pedestrian, bike and vehicular traffic. Paseos, landscaped areas and medians will provide unique visual interest through the use of ornamental grasses, informal tree groupings in addition to other water-conserving plant material in place of turf. Landscape along Encanto Drive provides visual interest from both the adjacent street and the I-215 freeway through the selection of large trees and a variety of plant material textures and colors.

#### **VILLAGE ENTRY STREET**

The Village Entry Street provides residents and visitors with an announcement of the arrival at a high-quality community. The landscape treatments of these entries provide both cohesion and accent through color, texture and form. Enhanced paseos along the Village Entry street serve as major pedestrian corridors and include sitting areas shaded by large trees. These paseos and entry medians will be accentuated through the use of ornamental trees, shrubs, grasses and ground covers. Distinctive specimen trees will accentuate a tot lot within both the east and west villages providing a safe, attractive place for children to play.

#### **LOCAL ROADS WITH AN ENHANCED PASEO**

Enhanced paseos traverse each Village in a north/south direction and connect the internal local streets. The enhanced paseo on one side of each collector serves as the major thematic element, connecting neighborhoods to all community amenities and the regional trail system. The landscape design for the enhanced paseos reinforces the Village entry experience and unifies the neighborhoods through the use of larger, uniformly spaced trees and a variety of plant material textures and color. The enhanced paseos will provide unique visual interest through the use of ornamental grasses and other water-conscious plant material. Appropriate lighting, signage and other crime prevention through environmental design (CPTED) strategies will be utilized in the design of the enhanced paseos. Should a part of the East Village become age-restricted, a section of the enhanced paseo road will be gated to restrict access for that portion of the community.

#### **LOCAL STREETS**

Local streetscapes will provide a cohesive neighborhood character and complement the design of the Village Entry Street. The use of color, texture and spacing of plant material should be carefully crafted to create an interesting and inviting pedestrian experience. Average tree spacing should not exceed 30 feet on center. Neighborhood entry points shall be emphasized through an enhanced landscape treatment of accent plant materials.

### **4.2.4 EDGE CONDITIONS**

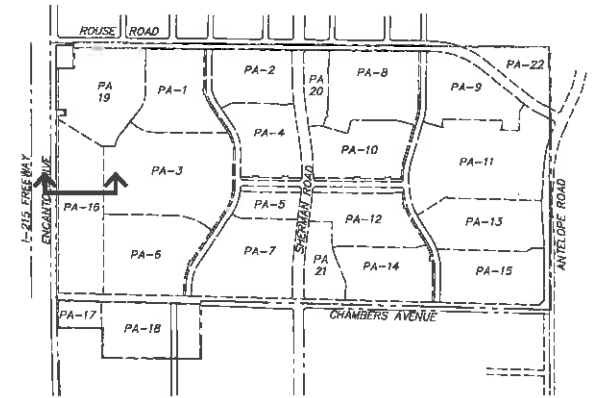
#### **COMMUNITY EDGE CONDITIONS**

Within Fleming Ranch, transitions between land uses will occur at the boundaries separating the existing and proposed residential and between the commercial uses.



### NEW RESIDENTIAL/EXISTING RESIDENTIAL EDGE CONDITIONS

New residential development along Rouse Road, Antelope Road and Chambers Avenue interfaces with existing and future residential developments. In all conditions, the two uses are separated by a road and a generous tree-lined landscape buffer is provided along the entire perimeter of the new residential development. A portion of the edge condition includes a water quality basin, providing significant visual relief. In addition, a 6-foot high block wall is planned for the boundary of the new residential development area for visual separation. Refer to Figure 4.9: Wall and Fence Plan.



### NEW RESIDENTIAL/COMMERCIAL INTERFACE

The proposed commercial planning area along the western boundary serves as a buffer between the proposed residential areas and the I-215 Freeway. Between the commercial and residential land use areas, a densely landscaped 20-foot wide buffer within the commercial area provides separation from the residential or park uses.

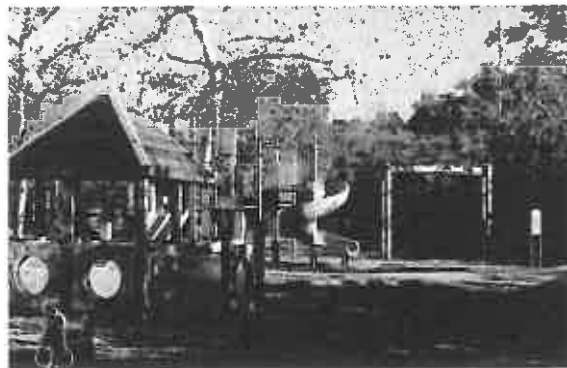


**FIGURE 4.7: COMMERCIAL DEVELOPMENT CROSS SECTION**

**SPORTS PARK**

A major public amenity for Fleming Ranch is the sports park located south of Chambers Avenue, adjacent to Hans Christensen Middle School. This lighted park is planned to feature sport fields, parking, restrooms, picnic areas and shade structures. A protected crosswalk will be provided at the Chambers Avenue and Sherman Road intersection to provide a pedestrian connection from the sports park to the enhanced paseos, encouraging walking and biking to the park and school. Landscape design for the park is to include a variety of shade trees to provide relief from high temperatures.

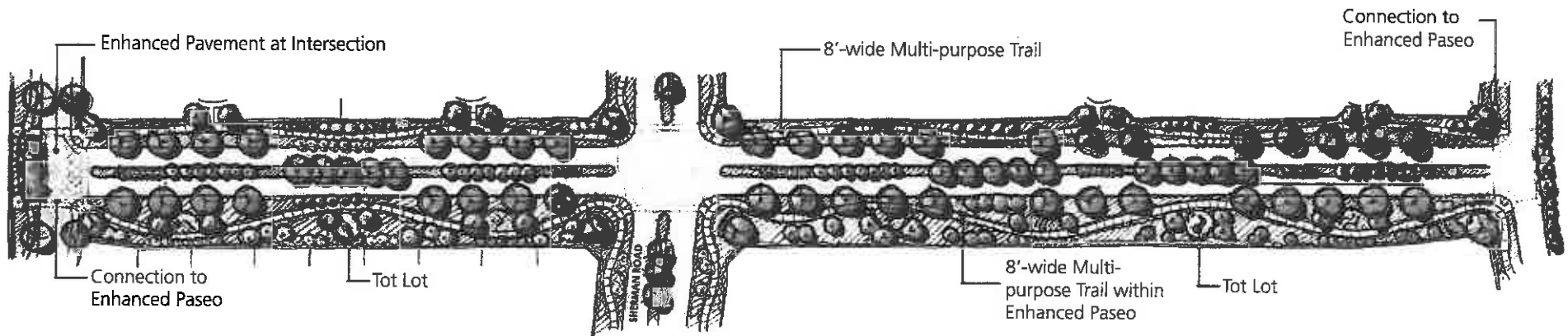
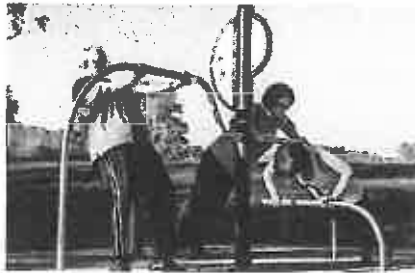
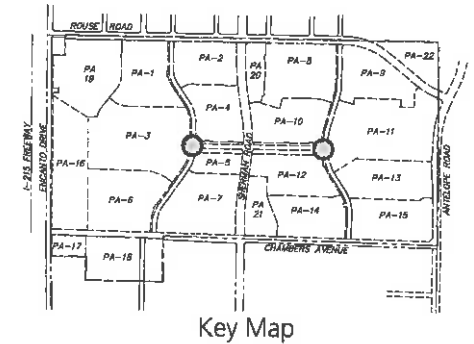
The design of the Park shall meet the requirement of the City of Menifee Trails, Parks, Open Space, and Recreation Master Plan published in February, 2016.



**4.2.5 COMMUNITY RECREATION**

The enhanced paseos throughout Fleming Ranch are the major amenity of the community, encouraging a healthy lifestyle for residents.

The design of the Park shall meet the requirement of the City of Menifee Trails, Parks, Open Space, and Recreation Master Plan published in February, 2016.



**FIGURE 4.8: VILLAGE ENTRY/ENHANCED TRAIL**

**ACTIVE ADULT OVERLAY RECREATION**

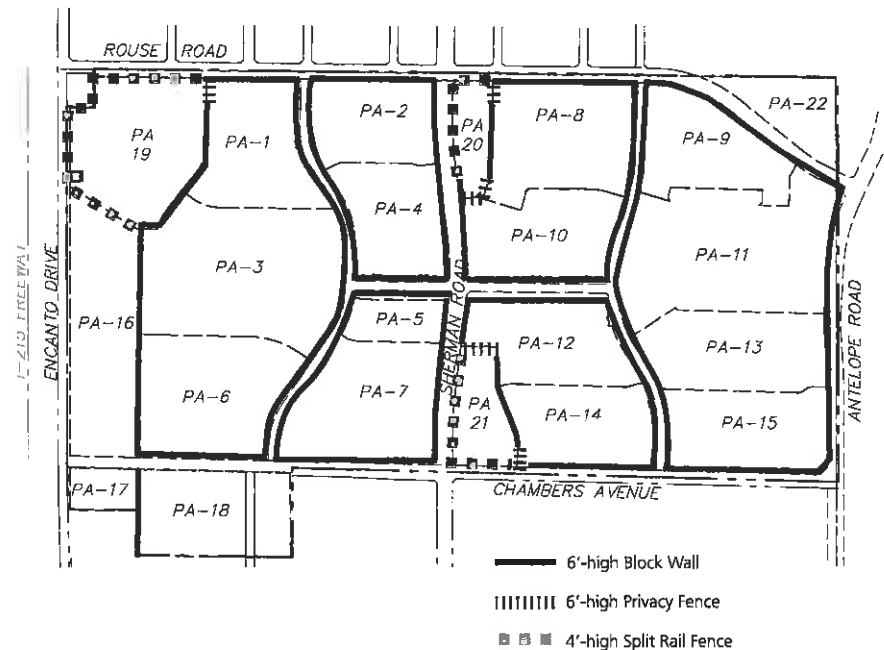
Should the age-restricted overlay be implemented, a private recreation area would be included featuring common outdoor recreational and social activities such swimming, bocce ball, horseshoes, shuffleboard and social gathering areas with picnic tables, barbecue grills and shade structures. In addition, indoor social activities would be provided within a community recreation building.

**4.2.6 WALLS AND FENCES**

Walls and fencing will be used throughout the community to complement the overall design theme, establish community identity, provide protection from roadway and other noise, and allow privacy and security in residential areas. The locations and details of walls and fences are provided in Figure 4.9: Wall and Fence Plan and Figure 4.10: Wall and Fence Details. The following is a list of general guidelines regarding walls and fences:

- ❖ All community theme walls and fences shall be consistent in design;
- ❖ Any fence/wall adjoining a public street or any fence/wall visible from the side or rear shall be the community theme wall;
- ❖ Vines and/or shrubs should be planted along community walls to soften the visual character. An extensive use of vines is encouraged;
- ❖ The maximum wall or fence height shall be six feet within any required rear, or side setback area, and along the project perimeter. Fence/wall heights are measured from the base of the fence/wall to the top of the interior or exterior side, whichever is greater, except in no case should the maximum height of the wall exceed ten feet (in combination with a retaining wall) unless required for pool safety; and
- ❖ Combination retaining wall and privacy walls at block ends may be used.

Three types of walls and fencing are planned; block walls, split rail fences and privacy fences. All walls and fences reinforce the Fleming Ranch community theme.



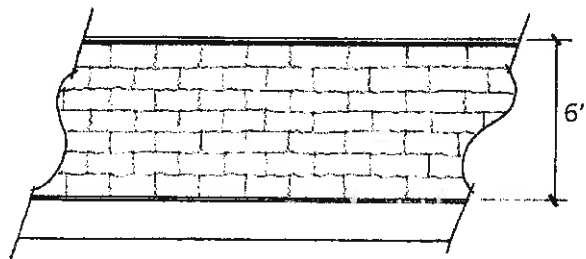
**FIGURE 4.9: WALL AND FENCE PLAN**

**BLOCK WALL**

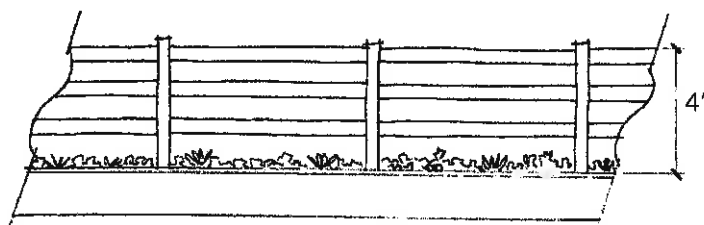
Block walls will be used for a variety of purposes, where privacy or security is desired, as well as in locations where screening is necessary. The block wall is six feet in height. Pilasters will be provided at regular intervals, especially at changes in direction or elevation. The use of ivy or other vegetative material is highly encouraged to reduce the likelihood of graffiti. Wall height shall not exceed six feet, unless necessary for noise attenuation or other special circumstances.

**SPLIT RAIL FENCE**

A four-foot high split rail fence will be located at the community entry locations adjacent to the detention basins. These fences serve as a screening element as well as reinforcing the overall theme. The fence will complement the design of the block walls and will be softened with trees and other vegetation.



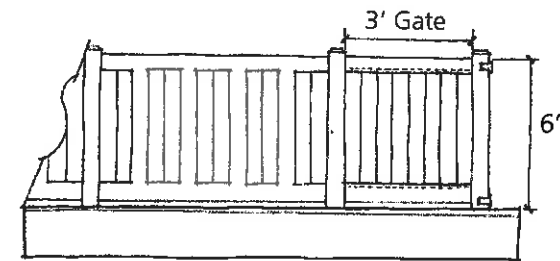
*Block Wall*



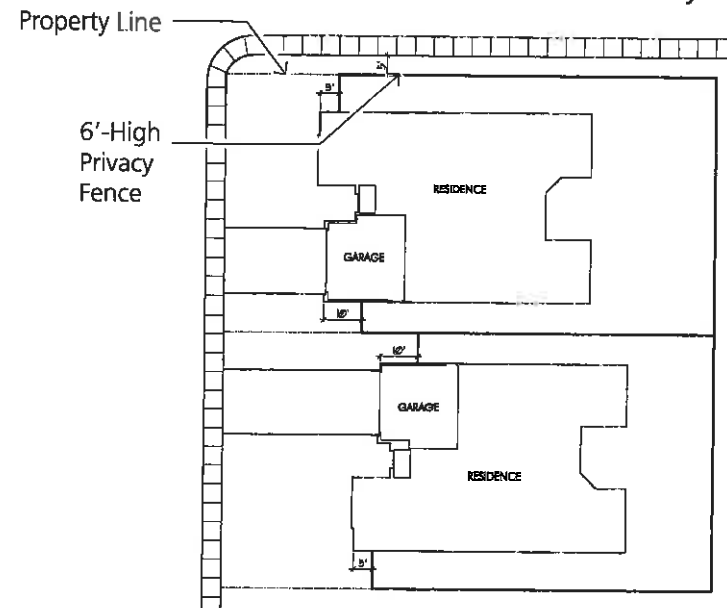
*Split Rail Fence*

**PRIVACY FENCE**

Six-foot high privacy fencing will be used for property line fencing and gate returns between homes. Fence returns located on the garage side of each home shall include a three-foot wide gate and should occur ten feet from the front of the house. Fencing on the living area side of the home should return five feet behind the front of the house. Where the fence return conflicts with window locations, the fence shall be located one-foot behind the window or centered between window space less than one-foot apart.



*Privacy Fence*



**FIGURE 4.10: WALL AND FENCE DETAILS**

#### **4.2.7 OUTDOOR LIGHTING**

All streets and recreational areas in Fleming Ranch shall have uniform lighting standards with regard to style, materials, and colors in order to ensure consistent design. Lighting fixtures shall be well integrated into the visual environment and the appropriate architectural theme. All lighting shall comply with the following regulations and provisions:

- ❖ Outdoor lights, such as streetlights, spotlights, floodlights, and reflector lights, shall be recessed or otherwise designed to reduce the problems associated with damage and replacement of fixtures;
- ❖ Fixtures shall be vandal-proof, yet should not look institutional;
- ❖ Streets and intersections shall be well-lit in accordance with the City standard illumination levels;
- ❖ Low-level lighting for pedestrian safety should be installed where appropriate;
- ❖ Accent lights should be installed at all primary entry monuments, secondary monuments and park/trail monuments;
- ❖ Street lights shall conform to the overall project theme;
- ❖ All exterior lighting for identification, pools, water features, and landscaping should be subdued and indirect to prevent spill over onto adjacent lots and streets;
- ❖ The type and location of building lighting should preclude direct glare onto adjacent property, streets and skyward;
- ❖ Pedestrian scale fixtures are encouraged over “high mast” poles;
- ❖ Consistent lighting fixtures shall be used throughout the SP area to enhance community character and,
- ❖ Park lighting (field and parking lots) shall be consistent with City of Menifee Park Development Guidelines.

#### **4.2.8 GENERAL LANDSCAPE REQUIREMENTS**

These landscape guidelines set criteria for landscaping within both residential and non-residential zones. The guidelines also ensure that a cohesive landscape framework will be created to unify the community at all levels of development. Landscaped areas within Fleming Ranch are to provide a unified concept consistent with the overall design theme, to provide identity for the various portions of the community, and to provide active and passive recreational opportunities. A Community Plant Palette has been provided within the Appendix. Additions to the landscape palette are allowed with administrative approval. To achieve the objectives of the community theme, the following criteria should be followed:

- ❖ Landscaping should complement the overall design theme through the careful use of flower and leaf color and texture, plant forms, and plant masses;
- ❖ Existing natural conditions and situations should be considered during the landscape design process;
- ❖ No single species should dominate the landscape palette. A variety of tree and shrub species should be specified while maintaining a consistent character and minimizing potential loss because of tree diseases;
- ❖ Landscaping along major roadways and entries should be consistent, formalized, and composed of signature plantings such as masses of shrubs or groupings of trees;
- ❖ Large deciduous trees should be planted in a regular spacing or pattern no further than 50 feet apart;
- ❖ Informal plantings of trees, shrubs, groundcovers, grasses, and vines should be planted between sidewalks and walls to soften their appearance;

- ❖ Plantings at entries and key intersections are encouraged to employ materials having a variety of heights and textures to enhance the visual impact of these areas. Flowering species and those with seasonal interest are encouraged at entries and key intersections;
- ❖ Consistent street tree themes should be related to the hierarchy of the street system; and
- ❖ Design of the shrub understory should incorporate a “layering” effect to create depth, variety and interest.

The following is a list of ‘sustainable’ features to be used and/or considered for the Fleming Ranch community:

- ❖ Encourage the use of low toxic wood preservatives (no CCA), or naturally rot-resistant wood for landscaping;
- ❖ Use appropriate sizes and thoughtful placing of plants prevents overgrowth and future thinning, reducing the amount of material sent to the landfill;
- ❖ Carefully locate trees and shrubs to ensure proper drainage and to reduce potential damage to buildings;
- ❖ Where possible re use soils from the site.
- ❖ Maintain and/or improve soil health through responsible management including: nurturing soil with organic matter and reducing synthetic fertilizer use;
- ❖ Implement integrated pest management to control or eliminate pesticide and toxic chemical use;
- ❖ Maximize tree cover to reduce energy demand, solar heat gain in buildings and to avoid absorption of heat by paved areas; and
- ❖ Consider sustainable choices site furnishing selections such as recycled materials, environmentally preferable products, materials that can be recycled, certified “green” products and locally available or manufactured products.

#### **4.2.9 PLANTING AND IRRIGATION GUIDELINES**

The following guidelines pertain to installation and maintenance of public landscaped areas (streetscapes, parks, and paseos). The following subsections provide information on general standards which should be followed in the design and installation of landscaping.

##### **A. LANDSCAPE PLANTING**

Because of the climate extremes in the project area, the installation of plant materials during the coldest winter months (December through March) or the hottest summer/fall months (July through September) should be avoided. If planting must be done during these periods, plant establishment may be difficult, and may require a prolonged period of time. In all installation, the use of plant materials acclimated to the project area should be encouraged.

- ❖ Use a simplified palette of plant materials which complements the overall theme;
- ❖ Avoid the use of many unrelated plant varieties in favor of broad plant masses and consistency of landscape character;
- ❖ Design masses of plant materials to complement architectural elevations and roof lines through color, texture, density, and form on both the vertical and horizontal planes;
- ❖ Space plant material on anticipated growth in order to promote natural forms without the need for excessive future pruning and maintenance;
- ❖ Use mulch wherever appropriate to conserve water and mitigate evapotranspiration;
- ❖ Select plant material that are known to have been successful in the area or in similar climatic and soil conditions;
- ❖ Avoid plant materials known to have invasive or destructive root systems and known to produce excessive litter or pose dangers due to falling limbs;

- ❖ Use landscape to screen utility enclosures or utility cabinets from view from major streets; and
- ❖ Select non-invasive, native vegetation types plant materials for the open space/drainage areas.

#### **B. LANDSCAPE IRRIGATION**

The following general irrigation concepts shall be considered in the design and installation of irrigation systems:

- ❖ Irrigation for slope areas shall not apply water at a precipitation rate over the water absorption capacity of the soil;
- ❖ Private irrigation systems should be designed to apply water slowly, allowing plants to be deep soaked and to reduce run-off;
- ❖ Drip systems are encouraged in all areas, except turf areas and small ornamental plantings;
- ❖ The location of controller boxes, valves, and other above-ground equipment, shall be incorporated into the overall landscaping design;
- ❖ Where above-ground equipment is provided, it shall be screened or otherwise removed from public view, to the extent possible;
- ❖ Irrigation scheduling should be adjusted quarterly to meet plant requirements;
- ❖ Sprinklers with proper nozzles shall be selected to provide water to the landscape that is compatible with their respective soils;
- ❖ If soil information is not available, utilize low precipitation sprinklers and program controller to minimize run-off; and
- ❖ The irrigation clock shall be programmed to operate during low water demand periods of the day, such as early morning.

#### **C. MAINTENANCE**

- ❖ All landscaped public and common areas shall be maintained in accordance with the best industry standards for professional landscape maintenance;
- ❖ Regular maintenance shall include watering, fertilization, mowing, edging, pruning, trimming, weeding, herbicide programming, pesticide programming, clean-up, and other on-going seasonal programmed maintenance functions;
- ❖ Replacement of dead or diseased plant materials originally approved shall be done on a routine basis;
- ❖ Automatic irrigation systems shall be routinely inspected and repaired and maintained in peak operation condition at all times; and
- ❖ All common areas and areas open to the public, including sidewalks, parking areas, and service areas, shall be routinely kept free of litter and debris.

#### **4.2.10 COMMUNITY PLANT PALETTE**

The Fleming Ranch Plant Palette in Appendix A has been developed to reinforce the community theme and create seasonal change by including a balanced mixture of deciduous and evergreen materials. Species on this list are considered drought tolerant or medium to low or very low water using and were chosen based on specific growth characteristics, including flower and foliage color, texture and form.

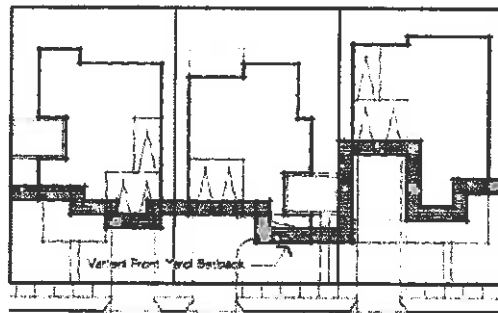


### 4.3 RESIDENTIAL SITE PLANNING

The following site planning design elements allow neighborhoods to ensure they meet the guiding principles.

#### 4.3.1 VARIED FRONT SETBACKS

Where possible, avoid plotting long rows of homes with the minimum garage and building setbacks. Provide varied front setbacks or articulate the front façade of the building at least five feet. Typically, plans should be reversed and plotted so that garages and entries are adjacent to each other to create an undulating setback. Occasionally, this pattern should be broken to avoid monotony or to best suit the individual lot and grading conditions.



*Undulating front building setbacks*

#### 4.3.2 SINGLE FAMILY NEIGHBORHOOD PLOTTING

Great neighborhoods are filled with diversity and variety of architecture that express the individuality of the owners while still creating compatibility and harmony in the neighborhoods. In choosing floor plans, styles (see the residential architectural styles subsection for approved styles) and color palette, the following criteria apply for each neighborhood.

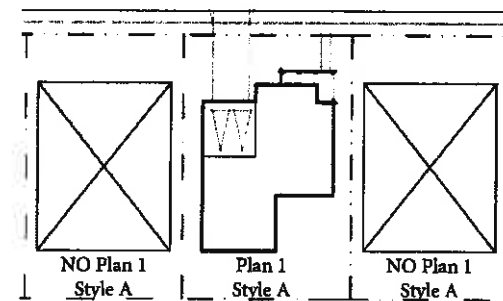
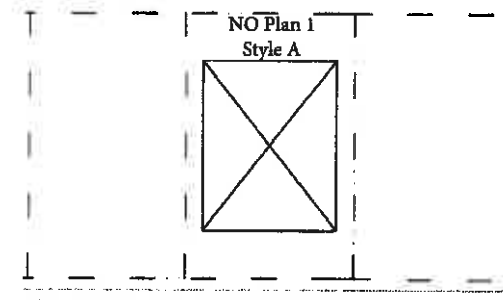
#### A. FLOOR PLAN PLOTTING

Each single-family subdivision with up to 50 units shall provide:

- ❖ Minimum three floor plans not including reversed plans;
- ❖ Three elevations for each floor plan using a minimum of two styles. If only two styles are selected, elevations shall be significantly different in appearance; and
- ❖ Four different color schemes for each floor plan.

Each single-family subdivision with between 51-99 units shall provide:

- ❖ Four floor plans not including reversed plans;
- ❖ Three elevations for each floor plan using a minimum of two styles. If only two styles are selected, elevations shall be significantly different in appearance; and
- ❖ Four different color schemes for each floor plan.



*Plan/ Elevational Style Plotting*

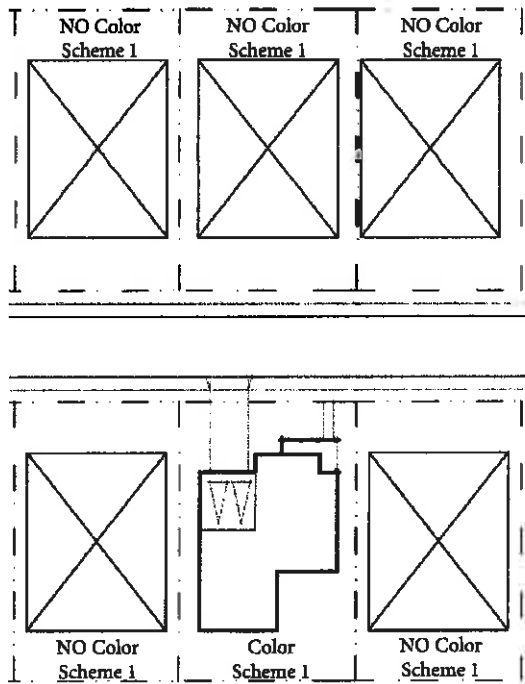
Each single-family subdivision with more than 100 homes shall provide:

- ❖ Four floor plans;
- ❖ Four elevations for each plan using a minimum of three styles. Elevations shall be significantly different in appearance; and
- ❖ Four different color schemes for each floor plan.

**B. STYLE PLOTTING**

To ensure that architectural variety occurs, the following criteria shall be utilized:

- ❖ Prohibit the same plan and elevation on the lot most directly across from each other and the one on either side of it.



*Color Scheme Plotting*

**C. COLOR CRITERIA**

To ensure variety of color schemes, like color schemes cannot be plotted adjacent to or immediately across the street from one another.

- ❖ Prohibit repeat of like color schemes (even if on a different floor plan) for the three lots most directly across from it and on the single lot to each side of it.

**4.4 ARCHITECTURAL REQUIREMENTS**

This subsection of the guidelines provides the design of residential buildings within Fleming Ranch.

**4.4.1 GUIDING PRINCIPLES**

The following residential guiding principles will guide the architecture to ensure quality development:

- ❖ Provide a varied and interesting streetscene;
- ❖ Focus the front elevation on the home, not the garage;
- ❖ Provide a variety of garage placements;
- ❖ Provide detail on rear elevations where visible from the public streets;
- ❖ Design homes that are simple in form and contribute to the charm and appeal of Fleming Ranch;
- ❖ Choose appropriate massing and roof forms to define the architecture styles;
- ❖ Ensure that plans and styles provide a degree of individuality; and
- ❖ Use architectural elements, details to reinforce individual architectural styles.

#### **4.4.2 EDGE CONDITIONS**

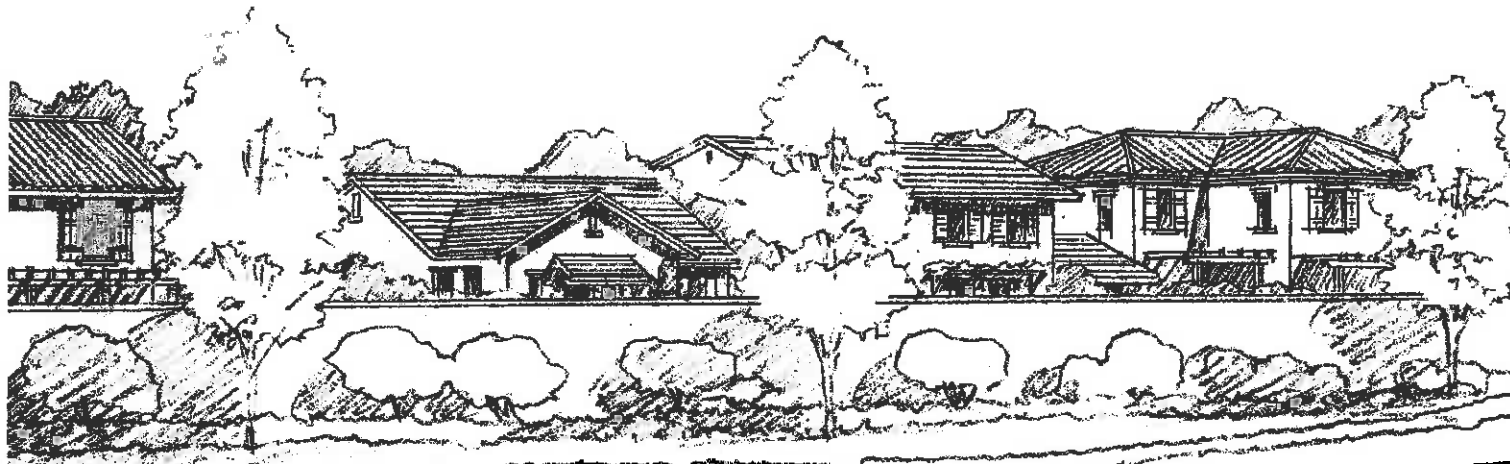
Rear elevations visible from open spaces and major roadways shall be treated in a manner respectful of their surroundings. Silhouettes and massing of homes along edges require design sensitivity. A row of homes with a single front or rear facing gable are prohibited. The following elements should be considered, and at least one element incorporated, in the design of side and rear elevations along edge conditions:

- ❖ A balance of hip and gable roof forms,
- ❖ Single-story plan,
- ❖ Single-story elements on two-story homes,
- ❖ Offset massing or wall planes (on individual plans or between plans),
- ❖ Roof plane breaks (on individual plans or between plans),
- ❖ A feature window (see page 4-23), or
- ❖ Detail elements similar to the front elevation.

#### **4.4.3 ROOF FORMS**

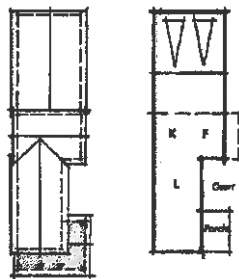
Rows of homes seen along major community roadways are perceived by their contrast against the skyline or background. The dominant impact is the shape of the building and roof line. To minimize the visual impact of repetitious flat planes, similar building silhouettes and similar ridge heights, discernibly different roof plans for each home plan shall be designed. Individual roof plans may be simple but, between different plans, should exhibit variety by using front to rear, side-to-side, gables, hipped roofs, and/or by the introduction of single story elements. The following roof design guidelines should also be considered:

- ❖ Provide a mix of gable and hip roofs along the streetscene;
- ❖ Design roofs for maximum solar exposure for the potential installation of solar features;
- ❖ Consider deep overhangs where appropriate to the style to provide additional shade and interior cooling;
- ❖ Consider light colored rooftops with a high solar reflectance to reduce the creation of a heat island effect and to maintain comfortable indoor air temperatures; and
- ❖ Offset roof planes, eave heights and ridge lines.



#### 4.4.4 SIMPLE HOUSE CONCEPT

In order to achieve authenticity of style in materials, detail, and execution while using resources efficiently, a more simple design must be considered in the crafting of the basic structure of the house. Straightforward massing and roof forms not only ensure efficient use of construction materials, but often lead to the most authentic expression of a style. The "Simple House" concept suggests that elementary structural forms and building masses can achieve authentic traditional styles promoted at Fleming Ranch. By shifting the emphasis from complex floor and roof plan designs and pop-outs and onto material application and architectural detailing that reinforce the architectural style of each home the public streetscene will be enhanced and our limited resources will be used efficiently.



*Simple house concept reflects simple floor and roof plans with added porch details*

- ❖ Enhance simple house design with appropriate colors, materials and details to keep the architectural style authentic;
- ❖ Use simple rectilinear forms as the basis for the floor plan. Add additional simple forms to expand, add interest, and achieve floor plan objectives;

- ❖ Select architectural styles that best fit the massing derived from the floor plan. For example, styles such as the Colonial Monterey and the American Traditional both elevate as a two-story rectangular form and can include a balcony or porch respectively; and



*Example of elevations that have similar massing*

- ❖ Use style appropriate architectural details to articulate wall planes, create shadow and provide visual interest.

#### 4.4.5 CORNER BUILDINGS

Buildings located on corners often times function as neighborhood entries and highlight the architecture for the overall Fleming Ranch community. Buildings located on corners shall include one of the following:

- ❖ Front and side façade articulation using materials that wrap around the corner-side of the building,
- ❖ Awning on corner side,
- ❖ Feature Window (see page 4-21) on corner side,
- ❖ Home entry on corner side,
- ❖ Corner facing garage,
- ❖ A pop-out side hip, gable or shed form,
- ❖ An added single-story element, such as a wrap-around porch or balcony,

- ❖ Recessed second- or third-story,
- ❖ Balcony on corner side, or
- ❖ Upgraded landscape.

#### **4.4.6 FRONT ELEVATIONS**

Front elevations shall be detailed to achieve variety along the street scene. Each front elevation shall incorporate a Feature Window treatment (see Feature Window requirements on the next page). In addition, each front elevation shall incorporate one or more of the following techniques:

- ❖ Provide enhanced style appropriate details on the front elevation;
- ❖ Offset the second story from the first level for a portion of the second story;
- ❖ Vary the wall plane by providing projections of elements such as bay windows, porches and similar architectural features;
- ❖ Create recessed alcoves and/or bump out portions of the building;
- ❖ Incorporate second-story balconies;
- ❖ Create interesting entries that incorporate features such as porches, courtyards, large recessed entry alcoves or projecting covered entries with columns; and
- ❖ Use a minimum of two building materials or colors on the front elevation.

#### **4.4.7 FEATURE WINDOWS**

All front and visible edge elevations shall incorporate one Feature Window treatment that articulates the elevation. Feature Window options include:

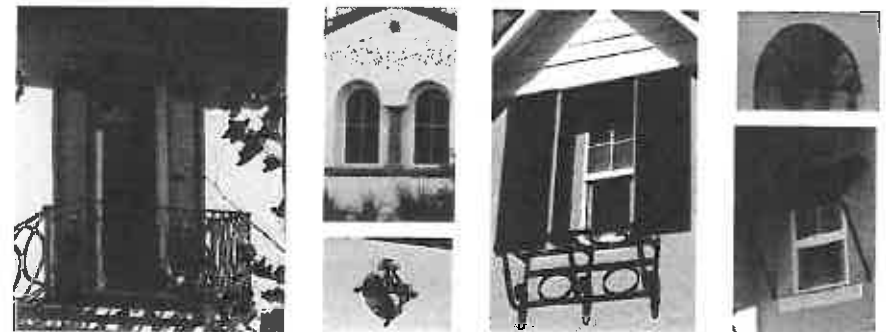
- ❖ A window of unique size or shape,
- ❖ Picture window,
- ❖ A bay window projecting a minimum of 24 inches, or a 12-inch pop-out surround,

- ❖ A window with a substantial surround matching or contrasting the primary color of the home,
- ❖ A window recess a minimum of 2 inches,
- ❖ Decorative iron window grilles,
- ❖ Decorative window shelves or sill treatments,
- ❖ Grouped or ganged windows with complete trim surrounds or unifying head and/or sill trim,
- ❖ A Juliet balcony with style-inspired materials,
- ❖ Window shutters, or
- ❖ Trellis protruding a minimum of 12 inches from the wall plan of the window.

#### **4.4.8. WINDOWS**

Windows on south-facing exposures should be designed, to the greatest extent possible, to maximize light and heat entering the home in the winter and to minimize light and heat entering in the summer.


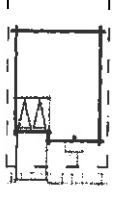

- ❖ West facing windows should be minimized or shaded where feasible to avoid the over heating of the homes.



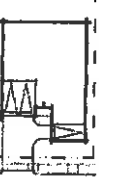





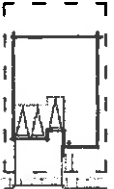
**4.4.9. GARAGE PLACEMENT & TREATMENT**

In order to create attractive and comfortable streetscenes and pedestrian spaces, the primary focus of front elevations should be on the living spaces of the home and not the garage. Garage placement should vary between building floor plans and may include the garage placements listed in Table 4.1: Garage Placements.

**TABLE 4.1: GARAGE PLACEMENTS**

GARAGE TYPES	REQUIREMENTS	EXAMPLE
FORWARD	<ul style="list-style-type: none"> <li>❖ Project at least 5' from a plane break in front of home (no flat facades)</li> <li>❖ Integrate the garage into the architectural design of the home</li> <li>❖ Recess garage door(s) a minimum of 8"</li> <li>❖ Vary the garage door treatment between forward facing garages within the neighborhood</li> </ul>	
SHALLOW RECESSED	<ul style="list-style-type: none"> <li>❖ Recess at least 5' from a plane break (no flat facades)</li> <li>❖ Integrate the garage into the architectural design of the home</li> <li>❖ Recess garage door(s) a minimum of 8"</li> <li>❖ Vary the garage door treatment between forward facing garages within the neighborhood</li> </ul>	
MID-RECESSED	<ul style="list-style-type: none"> <li>❖ Recess at least 10' from front living area</li> <li>❖ Integrate the garage into the architectural design of the home</li> <li>❖ Recess garage door(s) a minimum of 8"</li> </ul>	

GARAGE TYPES	REQUIREMENTS	EXAMPLE
DEEP RECESSED	<ul style="list-style-type: none"> <li>❖ Recess 20' from front living area</li> <li>❖ Integrate the garage into the architectural design of the home</li> <li>❖ Recess garage door(s) a minimum of 8"</li> </ul>	
SWING-IN	<ul style="list-style-type: none"> <li>❖ Integrate the garage into the architectural design of the home</li> <li>❖ Use the same architectural treatment on the street-facing garage walls as the front elevation</li> <li>❖ Include at least one street-facing window</li> <li>❖ Provide a back-up space of 28'</li> <li>❖ Recess garage door(s) a minimum of 8"</li> <li>❖ Vary the garage door treatment between forward facing garages within the neighborhood</li> </ul>	
SPLIT	<ul style="list-style-type: none"> <li>❖ Integrate the garages in the architectural design of the home</li> <li>❖ Provide a back-up space of 26'</li> <li>❖ Recess garage door(s) a minimum of 8"</li> </ul>	
TANDEM	<ul style="list-style-type: none"> <li>❖ Integrate the garage in the architectural design of the home</li> <li>❖ Recess garage door(s) a minimum of 8"</li> </ul>	

GARAGE TYPES	REQUIREMENTS	EXAMPLE
DETACHED REAR YARD	<ul style="list-style-type: none"> <li>❖ Use the same architectural design and roof style as the home</li> <li>❖ Recess garage door(s) a minimum of 8"</li> </ul>	
CORNER FACING	<ul style="list-style-type: none"> <li>❖ Use corner garage orientation for variety on corner lots</li> <li>❖ Integrate the garage into the architectural design of the home</li> <li>❖ Recess garage door(s) a minimum of 8"</li> <li>❖ Provide only one driveway per lot</li> </ul>	
THREE CAR FRONT FACING	<ul style="list-style-type: none"> <li>❖ Provide a maximum of two three-car front facing plans per neighborhood</li> <li>❖ Permitted only on lots 55 feet or wider</li> <li>❖ Offset a single garage door at least two feet from a double door; OR</li> <li>❖ Provide three single garage doors each separated by at least one foot</li> </ul>	

**4.4.10 GARAGE DOOR TREATMENTS**

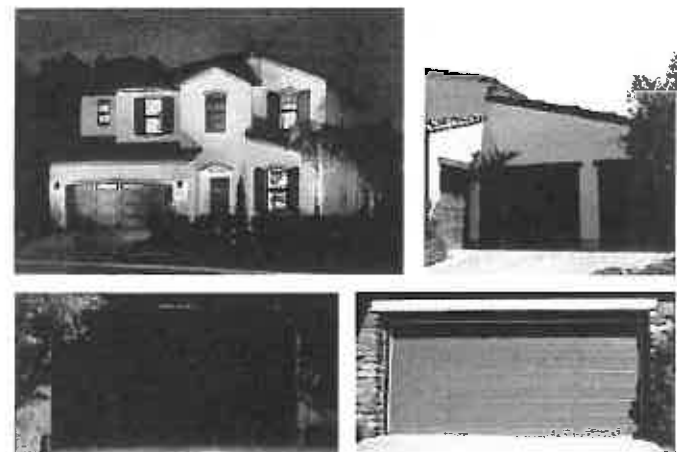
Appropriate treatment of garage doors further enhance the elevation and decrease the utilitarian appearance of the garage. Various garage door patterns, windows and/or color schemes should be utilized as appropriate to individual architectural styles.

- ❖ Garage doors shall be consistent with the architecture of the dwelling, to reduce the overall visual mass of the garage;
- ❖ Garage doors shall be recessed 8 inches from the wall plane;
- ❖ All garage doors shall be automatic section roll-up doors;
- ❖ When appropriate, single garage doors are encouraged; and
- ❖ Carriage-style garage doors of upgraded design are encouraged.

**A. STREET FACING GARAGES**

All street facing garages should vary the garage door appearance along the streetscene. Below are options for the door variety:

- ❖ Vary garage door pattern, windows and/or color as appropriate to individual architectural styles;
- ❖ Where possible, use two single doors instead of a large one.;
- ❖ Provide a porte cochere;
- ❖ Use an attached overhead trellis installed beneath garage roof fascia and/or above garage door header trim; or
- ❖ Span the driveway with a gated element or overhead trellis.



#### 4.4.11 BUILDING FORMS

Building form, detail and placement greatly effects how a structure is perceived based on how light strikes and frames the building. The effect of sunlight is a strong design consideration as shadow and shade can lend a sense of substance and depth to a building. The following elements and considerations can be used to facilitate the dynamic of light and depth perception of the building.

##### A. ARCHITECTURAL PROJECTIONS

Projections create shadow and provide strong visual focal points. This can be used to emphasize design features such as entries, major windows or outdoor spaces. Projections are encouraged on residential building forms. Projections may include, but are not limited to:

- ❖ Shutters,
- ❖ Awnings (cloth, metal, wood),
- ❖ Balconies,
- ❖ Eave overhangs,
- ❖ Projecting second- or third-story elements,
- ❖ Tower elements,
- ❖ Window/door surrounds,
- ❖ Recessed windows,
- ❖ Bay windows or dormers,
- ❖ Trellis elements,
- ❖ Shed roof elements, and
- ❖ Porch elements.

##### B. OFFSET MASSING FORMS

Front and street-facing elevations may have offset masses or wall planes (horizontally or vertically) to help break up the overall mass of a building.

- ❖ Offset forms are effective in creating a transition:
  - ❖ Vertically between stories, or
  - ❖ Horizontally between spaces such as recessed entries.
- ❖ Offset massing features are appropriate for changes in materials and colors;
- ❖ Offsets should be incorporated as a functional element or detail enhancement;
- ❖ Over-complicated streetscenes and elevations should be avoided; and
- ❖ Streetscenes should provide a mix of simple massing elevations with offset massing elements to compose an aesthetic and understandable streetscape.

##### C. LOWER HEIGHT ELEMENTS

Lower height elements are important to streetscene variety, especially for larger buildings or masses, as they articulate massing to avoid monotonous single planes. These elements also provide a transition from the higher story vertical planes to the horizontal planes of sidewalk and street, and help to transition between public and private spaces. Lower height elements are encouraged to establish pedestrian scale and add variety to the streetscene. Lower height elements may include but are not limited to:

- ❖ Interior living spaces,
- ❖ Porches,
- ❖ Entry features,
- ❖ Bay windows,
- ❖ Courtyards, or
- ❖ Pergolas.





**D. BALCONIES**

Balconies break up large wall planes, offset floors, create visual interest to the façade, add human scale to a building and provide outdoor living opportunities. Scaled second- or third-story balconies can have as much impact on stepped massing and building articulation as a front porch or lower height element. Balconies:

- ❖ May be covered or open, recessed into or projecting from the building mass;
- ❖ Shall be an integral element of, and in scale with, the building mass, where appropriate; and
- ❖ Are discouraged from being plotted side-by-side at the same massing level (i.e. mirrored second-story balconies).

**E. ROOF CONSIDERATIONS**

Composition and balance of roof forms are as definitive of a streetscene as the street trees, active architecture or architectural character.

- ❖ Rooflines and pitches, ridgelines and ridge heights should create a balanced form to the architecture and elevation;
- ❖ Direction of ridgelines and/or ridge heights should vary along a streetscene;
- ❖ Roof overhangs (eaves and rakes) may be used as projections to define design vocabulary and create light and shade patterns;
- ❖ Hip, gable, shed and conical roof forms may be used separately or together on the same roof or streetscene composition; and
- ❖ Roof form and pitch shall be appropriate to the massing and design vocabulary of the home.



#### 4.4.12 OUTDOOR OPEN SPACES

Outdoor living spaces can also create indoor/outdoor environments opening up the home to enhance indoor environmental quality. Outdoor open space shall meet the following requirements:

- ❖ Open to the sky,
- ❖ 10-foot minimum dimension, and
- ❖ 200 square feet minimum area.



#### 4.4.13 MATERIALS

The choice and use of materials has an important impact on the character of each neighborhood and the community as a whole. Wood is a material reflective of many architectural styles; however, maintenance concerns, a design for long-term architectural quality and new high-quality manufactured alternative wood materials make use of real wood elements less desirable. Where “wood” is referred to in these guidelines, it can also be interpreted as simulated wood trim with style-appropriate wood texture. In addition, some styles can be appropriately expressed without the wood elements, in which case stucco-wrapped, high-density foam trim (with style-appropriate stucco finish) is acceptable. Similarly, pre-cast elements can be satisfied by high-density foam or other similar materials in a style-appropriate finish.

- ❖ Wood, brick and stone cladding should appear as structural materials, not as applied veneers;
- ❖ Material changes should occur at logical break points;
- ❖ Materials applied to any elevation shall turn the corner of the building, ending at a logical termination point related to the roof line or building massing, or a minimum of 2 feet;
- ❖ Columns, tower elements and pilasters should be wrapped in the entirety;
- ❖ Siding is permitted to terminate at an outside corner where miter boards are used;
- ❖ Material breaks at garage corners shall have a return dimension equal to or greater than the width of the material on the garage plane elevation;
- ❖ Use durable roofing and siding materials to reduce the need for replacement; and
- ❖ Use local, recycled and/or rapidly renewable materials to conserve resources and reduce energy consumption associated with the manufacturing and transport of the materials.

#### **4.4.14 EXTERIOR STRUCTURES**

Exterior structures, including but not limited to, porches, patio covers and trellises, shall reflect the character, color and materials of the building to which they are related.

- ❖ In order to encourage porches, patio covers and trellis, these structures are not considered part of the maximum lot coverage requirements;
- ❖ Stairs should be compatible in type and material to the deck and landing;
- ❖ Columns and posts should project a substantial and durable image;
- ❖ Railings shall be consistent with the design vocabulary and be of appropriately scaled and durable materials; and
- ❖ Exposed gutters and downspouts shall be colored to complement or match the fascia material or surface to which they are attached.

#### **4.4.15 ACCESSORY STRUCTURES**

Accessory structures should conform to the design standards, setbacks and height requirements of the primary structure. If visible from the front or side lot line, the visible elevation should be considered a front elevation and should meet the design criteria of the applicable architectural style.

#### **4.4.16 LIGHTING**

Appropriate lighting is essential in creating an inviting evening atmosphere for the Fleming Ranch community. All lighting shall be aesthetically pleasing non-obtrusive and meet the dark sky requirements of Chapter 6.01 of the MMC.

- ❖ All exterior lighting shall be limited to the minimum necessary for safety;
- ❖ All exterior lighting shall be fully shielded to conceal the light source, lamp or bulb. Fixtures with frosted or heavy seeded glass are permitted; and
- ❖ Each residence shall have an exterior porch light at its entry that reflects the architectural style.

#### **4.4.17 UTILITY & MECHANICAL EQUIPMENT**

All utility and mechanical equipment should be located in the side and rear yards.

#### **4.4.18 ADDRESS NUMBERS**

Address numbers shall be lighted or reflective and easily visible from the street.

#### **4.5 RESIDENTIAL ARCHITECTURAL STYLES**

Fleming Ranch is envisioned as a sustainable, contemporary community where architectural massing, roof forms, detailing, walls and landscape collaborate to reflect, historic, regional, and climate-appropriate styles.

The design criteria established in this subsection encourages a minimum quality design and a level of style through the use of appropriate elements. Although the details are important elements that convey the style, the massing and roof forms are key to establishing a recognizable style. The proper scale and proportion of architectural elements and the suitable choice of details are all factors in achieving the architectural style.

The styles selected for the Fleming Ranch Specific Plan have been chosen from the traditional heritage of the southwest home styles, a majority of which have been influenced by the Spanish Mission and Mexican Rancho eras. Over the years, architectural styles in California became reinterpreted traditional styles that reflect the indoor-outdoor lifestyle choices available in the Mediterranean climate. These styles included the addition of western materials while retaining the decorative detailing of exposed wood work, wrought iron hardware and shaped stucco of the original Spanish styles. Mixing of style attributes occurs in both directions such as adapting Spanish detailing to colonial style form, or colonial materials and details to the Hacienda form and function.

The landscape and climate of California has also generated styles that acknowledge and blend with its unique setting. The Italian Villa is a good example of a transplanted style developed in a climate zone similar to the climate found in California.

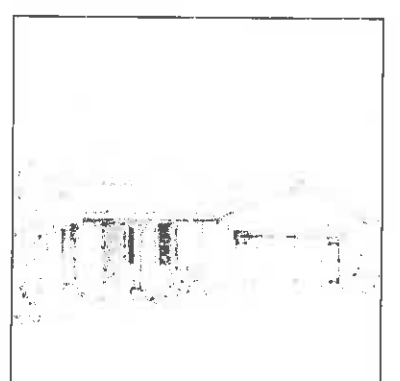
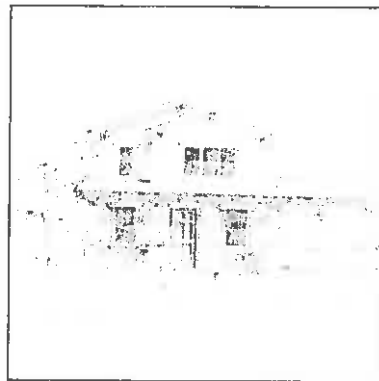
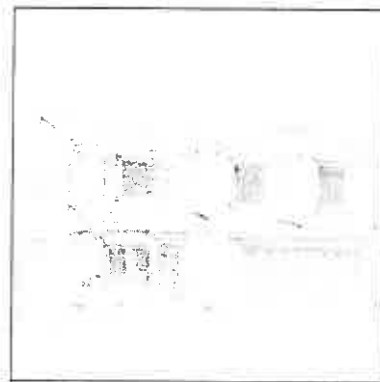
The following styles can be used in Fleming Ranch:

- ❖ American Colonial,
- ❖ American Traditional,
- ❖ Craftsman,
- ❖ Colonial Monterey,
- ❖ Farmhouse,
- ❖ Ranch, and
- ❖ Spanish Colonial.

The palette of styles listed above apply to all residential homes. Architectural styles may be added or deleted provided that they are compatible with the overall style palette and approved by the Planning Director of the City of Menifee

# SECTION 4

## DESIGN GUIDELINES



#### 4.5.1 AMERICAN COLONIAL

The initial source of the style originated from the work of the Adams brothers, British architects who drew heavily from their European travels to early Greek and Roman monuments. Their building forms reflected grand scale and vertical emphasis. The used classical elements such as porticos, columns and decorative motifs such as the swag and garland.

Second stories with overhangs, dormers and gabled roof forms became favored, evolving into classic elements of this traditional American style. Later, wings of smaller continuous gable forms were added to each side of the house. Wood shutters and white picket fences were the finishing details for an otherwise simple and functional form.



The primary form of this style is the two-story flat front box form. Other identifying features of this style include a monumented entry with decorative crown (pediment) supported by pilasters or columns projecting forward of the otherwise flat facade to form an entry porch. The entry is typically centered on the elevation. Windows are frequently placed in adjacent pairs flanking the entry in a symmetrical manner.

#### AMERICAN COLONIAL STYLE ELEMENTS:

- ❖ Plan form is typically a simple box with a flat facade;
- ❖ Roofs are typically of steeper pitch with shingles or flat concrete tiles and typical eave overhangs;
- ❖ Roof form is typically a front-to-back gable or singular hip;
- ❖ Stucco, brick and horizontal siding are typical wall materials; colonial style or brick trim is typically used as accents;
- ❖ The entry feature is typically traditionally pedimented with a substantial portico, stoop or surround;
- ❖ A round-top fan-light window may be used above the front door;
- ❖ Louvered shutters flanking fully trimmed windows are typical;
- ❖ Columns are typically simple and classical or, traditional with built up cap and base trim;
- ❖ The cornice is sometimes emphasized by dentils or decorative molding; and
- ❖ Bay windows are typical of this style.

**SECTION 4**  
**DESIGN GUIDELINES**



*Typical flat facade with Colonial detail*



*Pedimented entry shutters and window details*



*Colonial facade with shutters and entry portico*

#### 4.5.2 AMERICAN TRADITIONAL

The American Traditional style is a combination of the early English and Dutch houses found on the Atlantic coast. Their origins were sampled from the Adams and other classical styles. Details from these original styles are loosely combined in many examples.

Current interpretations have maintained the simple elegance of the early prototypes, but added many refinements and new design details. This style relies on its asymmetrical form and colonial detailing to differentiate it from the strict colonial styles.

Highly detailed entries having decorative pediments extended and supported by semi-engaged columns typically. Detailed doors with sidelights and symmetrically designed front facades. Cornices with dentils are an important feature and help identify this style.



#### AMERICAN TRADITIONAL STYLE ELEMENTS

- ❖ Plan form is typically asymmetric "L"-shaped;
- ❖ Roofs are typically of moderate to steeper pitch with shingles or flat concrete tile roof and exaggerated boxed eaves;
- ❖ Roof forms are typically hip or gable with dominant forward facing gables;
- ❖ Front facade is typically one solid material which may include stucco, brick, or shingle or horizontal siding;
- ❖ The front entry is typically sheltered within a front porch with traditionally detailed columns and railings;
- ❖ A curved or round-top accent window is typically used on front elevation;
- ❖ Windows are typically fully trimmed with flanking louvered shutters;
- ❖ Gable ends are typically detailed by full or partial cornice, sometimes emphasized by dentils or decorative molding; and
- ❖ Decorative or pedimented head and sill trim is typical.



**SECTION 4**  
**DESIGN GUIDELINES**



*Siding with American Traditional details*



*Entry porch, siding and shutters*



*Shutters with brick accents*



*Materials detail simple plan form*

### 4.5.3 CRAFTSMAN

Influenced by the English Arts and Crafts Movement of the late 19th century and stylized by California architects such as Bernard Maybeck in Berkeley and the Greene brothers in Pasadena, the style focused on exterior elements with tasteful and “artful” attention. Originating in California, Craftsman architecture relied on the simple house tradition, combining hip and gable roof forms with wide, livable porches and broad overhanging eaves. The style was quickly spread across the state and throughout the country by pattern books, mail-order catalogs and popular magazines.

Extensive built-in elements define this style, treating details such as windows and porches as if they were furniture. The horizontal nature is emphasized by exposed rafter tails and knee braces below broad overhanging eaves rustic-textured building materials. The overall effect was the creation of a natural, warm and livable home of artful and expressive character. Substantial, tapered porch columns with stone piers lend a Greene character, while simpler double posts on square brick piers and larger knee braces make a Craftsman distinctly more influenced by Maybeck’s designs



### CRAFTSMAN STYLE ELEMENTS

- ❖ Plan form is typically a simple box;
- ❖ Roofs are typically of shallower pitch with shingles or flat concrete tiles and exaggerated eaves;
- ❖ Roof forms are typically a side-to-side gable with cross gables;
- ❖ Roof pitch ranges from 3:12 to 5:12 typically with laminated shingles or flat concrete tiles;
- ❖ Wall materials may include stucco, horizontal or shingle siding and stone;
- ❖ Exposed rafter tails are typical under eaves;
- ❖ Siding accents at gable ends are typical;
- ❖ A front porch typically shelters the main entry;
- ❖ Porch columns can be done in a variety of distinctive ways. The following three options are typical of the Craftsman style:
  - ❖ Battered tapered columns (Stucco, brick or stone are typical),
  - ❖ Battered columns resting on brick or stone piers (either or both elements are tapered), or
  - ❖ Simpler porch supports of double square post resting on piers (Stucco, brick or stone are typical); piers may be square or tapered;
- ❖ Windows are typically fully trimmed; and
- ❖ Window accents typically include dormers or ganged windows with continuous head or sill trim.

**SECTION 4**  
**DESIGN GUIDELINES**



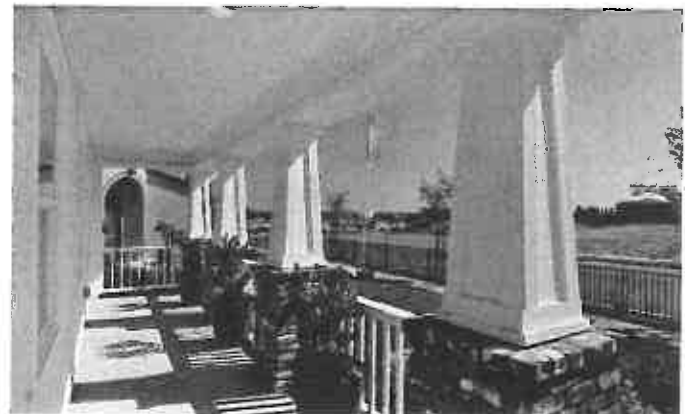
*Craftsman details*



*Exposed rafter tails and materials*



*Entry porch and Craftsman details*



*Porch with brick accents*

#### 4.5.4 COLONIAL MONTEREY

First built in Monterey, California by Thomas Larkin in 1835, the Colonial Monterey style introduces two-story residential construction and shingle roofs to California. The style was popularized by the use of simple building forms. Roofs featured gables or hips with broad overhangs, often with exposed rafter tails. Shutters, balconies, verandas and porches were integral to the Monterey character. Traditionally, the first and second stories had distinctly different cladding materials, typically with siding above and stucco and a brick veneer base below.

The introduction of siding and manufactured materials to the home building scene allowed for the evolution of the Monterey home from strictly Spanish Adobe construction to a hybrid of local form and contemporary materials. The composition of Spanish Colonial, Anglo and Greek Revival elements create a distinctly local flavor to a style that has been adapted and evolved throughout the United States. Siding, steeper pitched flat tile roofing and the cantilevered balcony elements on the Spanish Colonial house define this native California style.



#### COLONIAL MONTEREY STYLE ELEMENTS

- ❖ Plan form is typically a simple two-story box;
- ❖ Roofs are typically shallow to moderately pitched with shingles or flat concrete tile; "S" tile or barrel tile are also appropriate;
- ❖ Roof forms are typically a front-to-back gable with typical overhangs;
- ❖ Wall materials typically consist of stucco, brick or siding;
- ❖ Materials may contrast between first and second floors;
- ❖ A prominent second-story cantilevered balcony is typically the main feature of the elevation, two-story balconies with simple posts are also appropriate;
- ❖ Simple Colonial corbels and beams typically detail roof overhangs and cantilevers;
- ❖ Balcony or porch is typically detailed by simple columns without cap or base trim;
- ❖ Front entry is typically traditionally pedimented by a surround, porch or portico;
- ❖ Windows are typically accented with window head or sill trim of colonial-style and louvered shutters; and
- ❖ Corbel and post sometimes lean toward more "rustic" details and sometimes toward more "Colonial" details.

**SECTION 4**  
**DESIGN GUIDELINES**



*Rectilinear form and balcony with posts*



*Colonial-style detailed chimney*



*Elevation variation created by materials and cantilever*

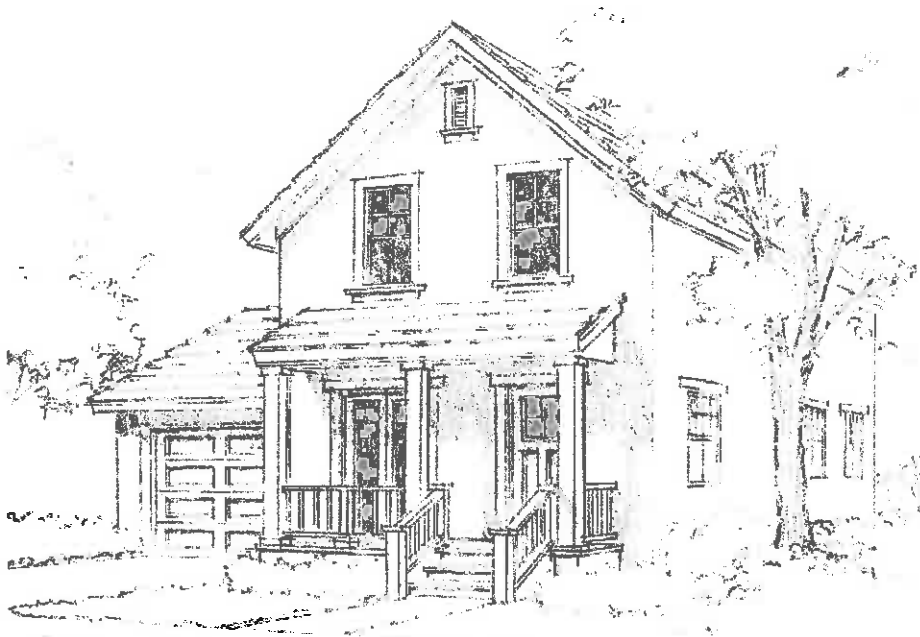


*Simple details*

#### 4.5.5 FARMHOUSE

The Farmhouse represents a practical and picturesque country house. Its beginnings are traced to both Colonial styles from New England and the Midwest. As the American frontier moved westward, the American Farmhouse style evolved according to availability of materials and technological advancements such as balloon framing.

Large front porches with a variety of wood columns and railings are the predominant feature of the style. Two story massing, dormers and a casual cottage look, with a more decorated appearance, is typical of the Farmhouse adaptations that spread through the West and California.



#### FARMHOUSE STYLE ELEMENTS

- ❖ Plan form is typically simple;
- ❖ Roofs are typically of steeper pitch with shingles or flat concrete tiles;
- ❖ Roof forms are typically a gable roof with front facing gables and typical overhangs;
- ❖ Roof accents sometimes include shed forms at porches;
- ❖ Wall materials may include stucco, horizontal siding and brick;
- ❖ A front porch typically shelters the main entry with simple posts;
- ❖ Windows are typically trimmed in simple colonial-style; built up head and sill trim is typical; and
- ❖ Shaped porch columns typically have knee braces.

# SECTION 4

DESIGN GUIDELINES



*Porch and materials indicative of style*



*Porch, posts, materials and roof form*



*Porch, shutters and trim*

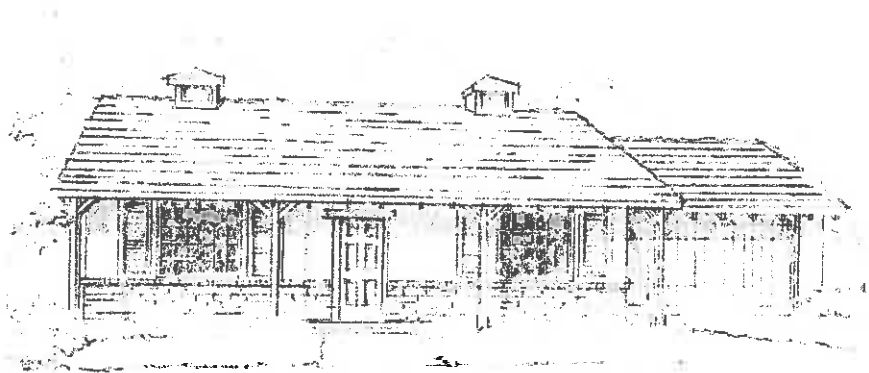


*Contrasting trim and porch*

### 4.5.7 RANCH

A building form rather than an architectural style, the Ranch is primarily a one-story rambling home with strong horizontal lines and connection between indoor and outdoor spaces. The “U”- or “L”-shaped open floor plan focused windows, doors and living activities on the porch or courtyard. The horizontal plan form is what defines the Ranch. The applied materials, style and character applied to the Ranch have been varied, adapted, interpreted and modernized based on function, location, era and popularity.

This single-story family oriented home became the “American Dream” with the development of tract homes in the post-World War II era. Simple and affordable to build, the elevation of the Ranch was done in a variety of styles. Spanish stylings with rusticated exposed wood beams, rafter tails under broad front porches and elegantly simple recessed windows were just as appropriate on the Ranch as the clean lines of siding and floor to ceiling divided-light windows under broad overhanging laminate roofs.



Details and elements of the elevation of a Ranch should be chosen as a set identifying a cohesive style. Brick and stucco combinations with overly simple sill trim under wide windows with no other detailing lends a modern Prairie feel while all stucco, recessed windows and exposed rusticated wood evokes a Hacienda ranch.

#### RANCH STYLE ELEMENTS

- ❖ Plan form is typically one-story of strong horizontal design;
- ❖ Roofs are typically shallow pitched with “S” tile, barrel tile, shingles or flat concrete tile;
- ❖ Roof forms are typically gable or hip with exaggerated overhangs;
- ❖ Wall materials typically consist of stucco, siding or brick;
- ❖ A porch, terrace or courtyard is typically the prominent feature of the elevation;
- ❖ Exposed rafter tails are typical;
- ❖ Porch is typically detailed by simple posts/beams with simple cap or base trim;
- ❖ Front entry is typically traditionally pedimented by a surround, porch or portico;
- ❖ Windows are typically broad and accented with window head and sill trim, shutters or recessed; and
- ❖ A strong indoor/outdoor relationship joined by sliding or French doors or bay windows is typical.



**SECTION 4**  
**DESIGN GUIDELINES**



*Simple details and massing*



*Horizontal one-story with porch*

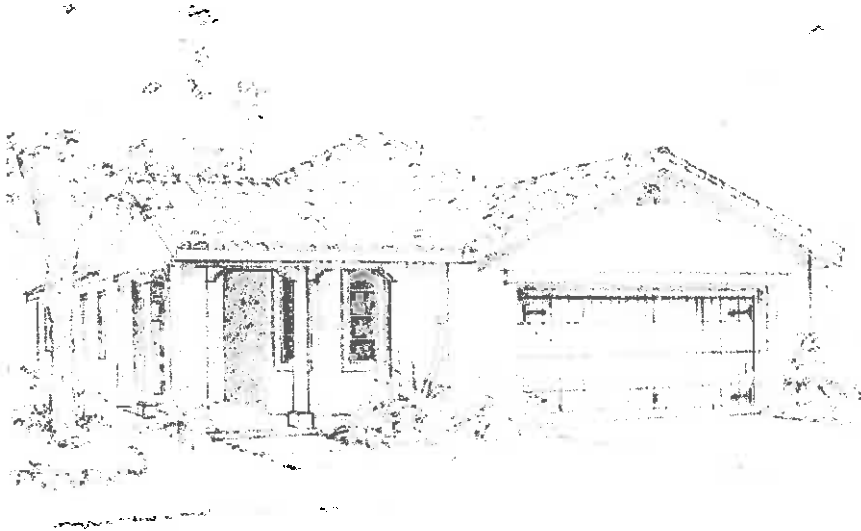


*Simple detailing with brick accents*

#### 4.5.8 SPANISH COLONIAL

This style evolved in California and the southwest as an adaptation of Mission Revival infused with additional elements and details from Latin America. The style attained widespread popularity after its use in the Panama-California Exposition of 1915.

Key features of this style were adapted to the California lifestyle. Plans were informally organized around a courtyard with the front elevation very simply articulated and detailed. The charm of this style lies in the directness, adaptability and contrasts of materials and textures.



#### SPANISH COLONIAL STYLE ELEMENTS:

- ❖ Plan form is typically a rectangular or “L”-shaped;
- ❖ Roofs are typically of shallower pitch with “S” or barrel tiles and typical overhangs;
- ❖ Roof forms are typically comprised of a main front-to-back gable with front-facing gables;
- ❖ Wall materials are typically stucco;
- ❖ Decorative “wood” beams or trim are typical;
- ❖ Segmented or full-arch elements are typical in conjunction with windows, entry or the porch;
- ❖ Round or half-round tile profiles are typical at front-facing gable ends;
- ❖ Arcades are sometimes used;
- ❖ Windows may be recessed, have projecting head or sill trim or be flanked by plank-style shutters; and
- ❖ Decorative wrought-iron accents, grille work, post or balcony railing may be used.

**SECTION 4**  
**DESIGN GUIDELINES**



*Gable form and arch windows define the style of this multi-family building*



*Typical form and arched entry detail*



*Spanish form and details*



*Spanish Colonial arcade and details*

## 4.6 NON-RESIDENTIAL GUIDELINES

This subsection provides general design guidance for the non-residential uses at Fleming Ranch. The guidelines:

- ❖ Define the character and quality of non-residential uses;
- ❖ Promote the human and pedestrian scale to ensure compatibility between non-residential and residential uses;
- ❖ Strengthen the pedestrian environment and improve overall community connectivity.
- ❖ Minimize potential negative visual impacts from the scale, bulk and mass inherent in large non-residential buildings;
- ❖ Minimize negative impacts to adjoining uses;
- ❖ Allow flexibility to respond to market conditions; and
- ❖ Promote site building and landscape design that are consistent with the commitment to sustainability.

### 4.6.1 SITE PLANNING

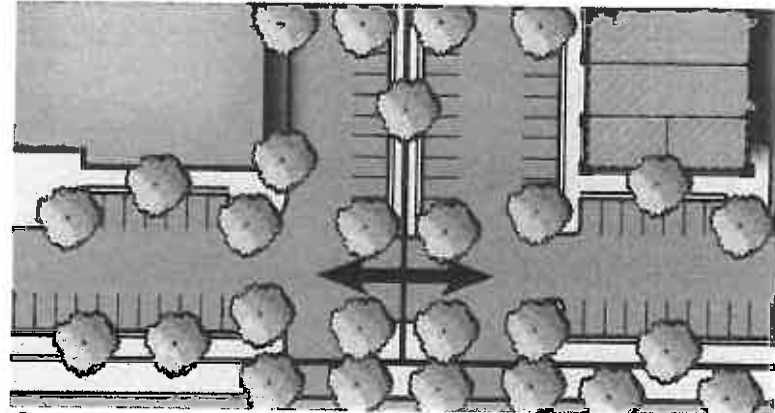
The following site planning design elements promote aesthetic quality, efficient use of the land, environmental responsibility and public safety.

#### A. CONNECTIVITY

The non-residential planning areas should be designed to allow for the safe and convenient movement of pedestrians, bicycles and vehicles:

- ❖ Provide clearly delineated pedestrian paths from perimeter sidewalks or trails to the building's main entrance;

- ❖ Encourage individual parcels to make internal connections to adjoining non-residential parcels to encourage walking instead of driving to the same destination; and
- ❖ Locate accessible bicycle parking near the building's main entrance.

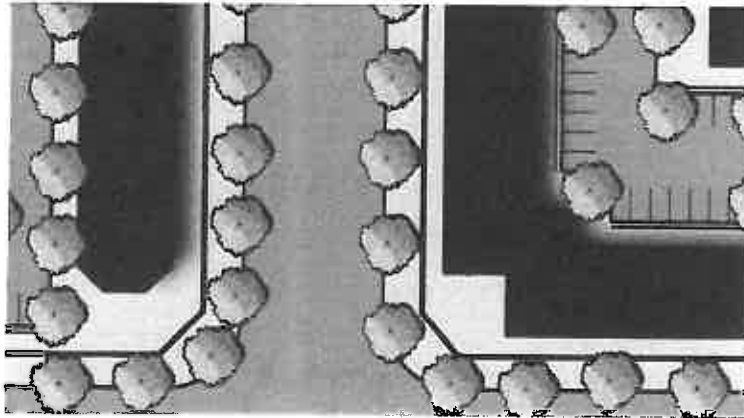


*Connect parcels*

#### B. BUILDING PLACEMENT/ORIENTATION

Special attention should be paid to the impacts of visibility, massing, and the height of the building. Consider the following elements during site design:

- ❖ Orient buildings to establish positive relationships with the adjacent streets;
- ❖ Locate buildings to frame and enclose interesting outdoor gathering spaces;
- ❖ Hide service and loading areas from view of the street where practical;
- ❖ Face primary entrances to off-street parking areas;
- ❖ Make building entrances clearly visible and easily identifiable as visitors access the site; and
- ❖ Provide well-defined pedestrian connections from the parking areas to the building entrances.



*Orient buildings to street*

**C. SITE AMENITIES/PEOPLE GATHERING PLACES**

People gathering places are encouraged to promote a vibrant and interactive environment for residents, employees and visitors alike. Common use areas may include (but are not limited to): plazas, outdoor eating areas, building entry forecourts and courtyards. These places provide opportunities for activities such as outdoor eating, casual meetings and small group gatherings.

- ❖ Arrange buildings to create and enclose a variety of outdoor people gathering places;
- ❖ Design people gathering places large enough to be usable, however not so large as to appear empty or barren;
- ❖ Furnish people gathering places with appropriate site amenities such as benches, low walls, shade trees, shade structures, water elements and bollards to facilitate pedestrian uses;
- ❖ Accommodate solar orientation for people gathering places to allow sunny outdoor spaces in winter and shade in the summer;

- ❖ Activate the pedestrian environment by interactive architecture and landscape including:
  - ❖ Architecturally vibrant storefronts,
  - ❖ Benches and planter walls for seating opportunities,
  - ❖ Fountains, murals, or public art, and
  - ❖ Accent or festive lighting to enhance nighttime ambiance.

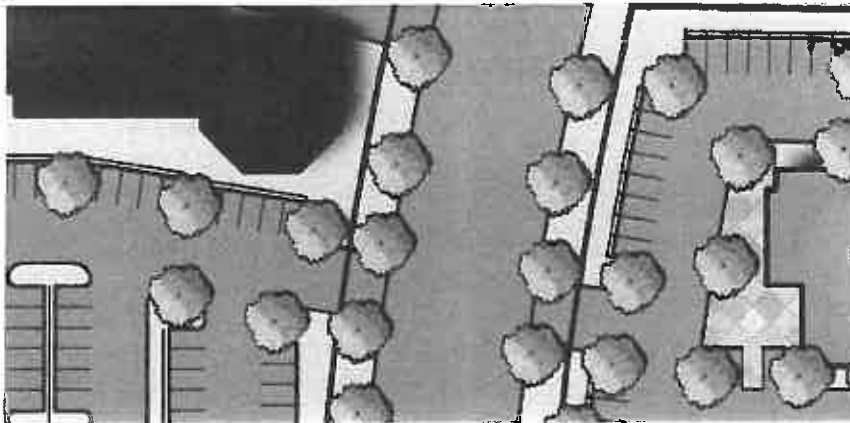


*Provide people gathering spaces*

#### **D. ACCESS AND SITE CIRCULATION**

Driveway access to parcels should provide safe vehicular movement and prevent traffic congestion as follows:

- ❖ Design driveways and parking areas to provide sufficient vehicular stacking during peak areas; and
- ❖ Provide a setback of 30 feet from the street curb to the first parking stall perpendicular to a driveway or to the first drive aisle intersection.



*Provide safe access*

#### **E. BUFFERS**

Incorporate visual buffers, including landscaping, equipment and storage area screening, and roof treatments.

#### **F. PARKING**

Sufficient employee and visitor parking shall be provided. However, individual developments are encouraged to seek opportunities, incorporate design features, or transportation management strategies that include shared parking to reduce automobile use.

- ❖ Provide convenient locations for carpool and bicycle parking;
- ❖ Screen parking areas from view of public streets by either walls, berms and/or planting materials; and
- ❖ Reduce the heat island effect by providing shade canopies and shade trees.

#### **G. SIGNAGE**

Lighting should be incorporated into signage design when appropriate in order to minimize glare and light spillage while accentuating the design of the signage.

- ❖ Integrate project signage into the architectural design and character of new buildings.
- ❖ Discourage the use of flashing, moving or audible signs.

**H. UTILITIES, SERVICES & REFUSE COLLECTIONS**

Utilities, services and loading areas should be provided to service the business and activities conducted on the parcel.

- ❖ Locate above ground utility facilities so they are not highly visible from the street or pedestrian routes;
- ❖ Screen utility cabinets and pedestals with landscape where possible;
- ❖ Cluster the utility infrastructure where possible, and screen with landscape materials, berms, walls and/or other architectural elements;
- ❖ Screen all rooftop equipment and communication equipment from abutting roadways by parapet walls or roof structures;
- ❖ Locate service and refuse collection areas within interior, side or rear yards oriented away from public view; and
- ❖ Screen service loading area and refuse enclosures by a solid wall with materials of appropriate color and texture compatible to the adjoining building.

**4.6.2 ARCHITECTURAL GUIDELINES**

Non-residential areas are to be visually attractive and cohesive with the surrounding residential and natural environment. The successful creation of pedestrian-friendly, non-intrusive development can be achieved by implementing the following:

- ❖ Be scaled appropriately and authentic to the location and use of the building;
- ❖ Present a unified development character without creating repetitious or redundant forms or design;
- ❖ Be complementary to the color of architectural features of the Fleming Ranch community;
- ❖ Avoid singular building forms through the use of architectural elements, offset wall planes or changes in building massing/height;
- ❖ Highlight and accentuate entries through architectural elements or details such as materials, color, massing or similar;



# FLEMING RANCH

## SPECIFIC PLAN

- ❖ Utilize textured forms, sand blasting or scoring for visual relief on tilt-up panels. Smooth panels, without the above elements, may be used in conjunction with color variation;
- ❖ Finish metal panels, elements or wall systems to reduce reflection and glare; and
- ❖ Orient loading and storage areas away from major roadways or residential edge conditions. Where this is not feasible, appropriate shielding should be used to blend with site design vocabulary.

### A. BUILDING DETAILS

Articulate forms with layered wall planes, banding, architectural details and/or materials. At least two of the following techniques should be used to enhance building architecture and reduce overall mass:

- ❖ Color variation,
- ❖ At least two different materials,
- ❖ Change in texture,



- ❖ Vertical/horizontal wall plane projections/recesses (minimum 2-foot offset),
- ❖ Variation of roofline (height or form),
- ❖ Revealed pilasters,
- ❖ Architectural elements significantly different from main building in mass or height,
- ❖ Trellis or awning element (proportional to massing of building),
- ❖ Balconies, or
- ❖ Aesthetic window groupings or treatments.



**B. ROOF CONSIDERATIONS**

Roofs should be designed for functionality and enhance/complement the overall architectural design of the building. The following design elements should be considered:

- ❖ Encourage vertical roof plane breaks, changes in building/ridge height or other accent roof forms;
- ❖ Integrate form and materials with the overall design vocabulary of the development;
- ❖ Use fascia and/or cornice elements that are consistent with the primary design; and
- ❖ Use contiguous parapets, when used, and incorporate side/rear elevation returns to eliminate false front/unfinished appearance.



**C. FACADE TREATMENTS**

Building should have articulation along auto and pedestrian corridors to generate pedestrian scaling and visual interest along the streetscene.

- ❖ Avoid blank walls, especially along the primary pedestrian walkway and street frontages;
- ❖ Detail buildings that use only one building material with banding, architectural details, textures, color variation and/or offset massing;
- ❖ Provide shadow articulation and scale to building elevation through projections, overhangs and recesses; and
- ❖ Unify architectural design for all pedestrian or major roadway elevations.

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# SECTION 5



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## ADMINISTRATION & IMPLEMENTATION

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This SP will be implemented through the processing of numerous discretionary entitlements. The implementation process provides the mechanism for reviewing precise development plans and ensuring development consistency with the SP's objectives. This Section also provides procedures for determining substantial conformity and, if necessary, amendments to the SP. All development is subject to the implementation procedures described in this Section. Additional information on implementation, including potential funding mechanisms, maintenance responsibilities, and monitoring activities are also presented.

Pursuant to Government Code Section 65451, all SP's must contain a "program of implementation measures including regulations, programs, public works projects, and financing measures" necessary to implement the Specific Plan.

## **5.1 ADMINISTRATION**

### **5.1.1 RESPONSIBILITY**

The Director shall be responsible for the administration and enforcement of the SP in accordance with the provisions herein, the State of California Government Code, and the Subdivision Map Act, including: processing assistance, interpretations of provisions, approval of administrative permits, issuance of permits, site development plans, approval of temporary or interim uses, specification of conditions of approval, and authorization of certificates of occupancy for new development.

The Planning Commission shall be responsible for recommending approval to the City Council regarding any subdivision, conditional use permit, or variance application; recommending SP amendments to the City Council; and acting on appeals from decisions by the Director.

The City Council shall be responsible for approving or denying amendments to the SP and acting on appeals of decisions by the Planning Commission.

### **5.1.2 APPLICABILITY**

Development of Fleming Ranch shall be implemented consistent with the SP goals, policies, and standards in combination with applicable City rules, regulations, and policies. Whenever provisions and development standards contained herein conflict with those of the City of Menifee's Zoning Code (ZC), the provisions of this SP shall prevail. In the event that the SP remains silent on an issue, the ZC shall prevail.

### **5.1.3 SEVERABILITY**

If any Section, subsection, sentence, clause or phrase of this SP or future amendments or additions hereto, is for any reason held to be invalid or unconstitutional by the decision of any court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this Plan.

### **5.1.4 INTERPRETATION**

If there is a question or ambiguity regarding the interpretation of any provision of this SP, the Director has the authority to interpret the intent of the provision, using the spirit and intent of this SP as a guide.

The Director may, at his/her discretion, refer interpretations to the Planning Commission for consideration and action. Such a referral shall be accompanied by a written analysis of issues related to the interpretation. All interpretations made by the Director and decisions of the Planning Commission may be appealed per applicable provisions of the ZC.

## **5.2 SP MODIFICATIONS**

Modifications to the text and exhibits may be necessary during the development of a project. Any modifications to the SP shall occur in accordance with the amendment process described below. Depending on the nature of the proposed amendment, additional analysis or a supplemental EIR may be required, pursuant to the California Environmental Quality Act (CEQA).

### **5.2.1 CLASSIFICATION**

Changes to the adopted SP shall be classified by the Director as either a Substantial Conformance or an Amendment. An applicant shall submit a detailed justification explaining why a Substantial Conformance revision or an Amendment is warranted and any exhibits deemed necessary by the Director.

#### **A. SUBSTANTIAL CONFORMANCE**

Substantial conformance allows for the administrative approval and interpretation of minor modifications to text, graphics, and/or project design that do not change the meaning or intent of the SP. Through the review and approval process, a project may be found in substantial conformance with the provisions of this SP and may be approved, conditionally approved, or denied by the Director under the circumstances listed below, with input from relevant departments if necessary. The Director shall also have the discretion to refer any such request for substantial conformance to the Planning Commission for interpretation and action:

- ❖ Simple edits or clarifications to text, graphics or figures that do not change the meaning or intent of the SP;
- ❖ Revisions in the configuration, orientation, and size of building footprints, parking areas, recreational amenities, drainage areas, and landscape areas;
- ❖ Shifts in internal road alignments, widths, streetscape amenities, and access points that would not substantially alter the land use or circulation system;
- ❖ Changes to the locations and sizes of infrastructure systems, including drainage, grading, water, and wastewater plans that would not substantially alter the plans, provided the changes can be supported by technical studies reviewed and approved by the City;
- ❖ Modifications of design elements such as paving treatment, colors, architectural details, signs, landscaping, fencing, lighting, and entry treatments as long as the Director finds the change to be compatible with previous developments/approvals;

- ❖ Changes to the Phasing Plan provided that the Director determines that infrastructure is available and constructed to serve that phase and that any mitigation measures linked to that phase, location, or level of development are implemented;
- ❖ A new type of land use that is not specifically addressed but that is similar in character and intensity to those listed in the SP; and
- ❖ Shifts in the number of dwelling units between villages that does not increase the overall number of dwelling units for either Village.

#### **B. AMENDMENTS**

Amendments as defined in this SP, shall be processed according to the provisions of the ZC. An amendment includes any of the following:

- ❖ Changes to exhibits or text that alter the intent of the SP;
- ❖ Changes to development standards and/or design guidelines, which, if adopted, would substantially change the physical character of the SP;
- ❖ A new type of land use that is not specifically discussed in this SP and that is not of the same intensity and character;
- ❖ Any change that would trigger the preparation of a supplemental EIR; and
- ❖ Changes in land use boundaries that result in an increase of more than the maximum allowable development potential, as analyzed in the certified EIR prepared for this SP.

### **5.3 REVIEW AND APPROVAL PROCESS**

Approval of the Fleming Ranch SP indicates acceptance by the City of Menifee City Council of a general framework of development for the approximate 331-acre site. Part of that framework establishes specific development standards that constitute the zoning regulations for this SP. It is further anticipated that

this SP will be implemented through a series of tentative tract maps, tract maps, and plot plans which shall be reviewed and approved by the Director and the appropriate hearing body to ensure consistency with this SP.

### **5.3.1 PRE-APPLICATION CONFERENCE**

A pre-application conference with the Director should be held before an application for a proposed project can be filed and accepted for processing. Representatives from the various City departments may be invited to attend the conference to provide input at the discretion of the Director. Multiple meetings may require the submittal of a deposit to cover staff time.

### **5.3.2 SUBDIVISION MAPS AND FINAL MAPS**

Subdivision maps can implement a SP by subdividing land into smaller parcels. The City of Menifee adopted Riverside County Ordinance No. 460 which includes a comprehensive list of required information for subdivision maps.

The subdivision map process may involve the preparation of a Tentative Parcel Map (TPM) and a Tentative Tract Map (TTM). The intent of the TPM is for financing and land conveyance purposes only—no infrastructure improvements, building and/or grading permits shall be issued for lots within the TPM. The individual planning areas will require a TTM showing each planning area, internal lots and street layout. The TTM may be prepared by the developer and/or the builder. In the absence of a specific builder, the master developer may choose to prepare the site plan and TTM to accommodate a specific size of home site. Additionally, an applicant may choose to file a conveyance or financing map and receive tentative map approval and record a final conveyance map.

During the TTM stage of the development process, the final number of dwelling units for a particular planning area may differ from those identified in the SP, so long as the density falls within the range specified by the land use designation. Furthermore, an individual TTM may fall outside of the specified density range, so long as the total density for a particular planning area falls within the range specified by the land use designation. However, the overall number of dwelling units may not increase.

After a subdivision map receives its tentative approval, the applicant is given a period of time to provide the final improvement plans for streets, utilities, grading, landscaping and all final conditions of approval prior to commencing construction.

### **5.3.3 ADMINISTRATIVE REVIEW AND PLOT PLANS**

Applications that comply with the provisions of the SP and do not require the approval of a public use permit or conditional use permit may be approved or conditionally approved by the Director through approval of a plot plan. Uses requiring the approval of a plot plan are identified on Permitted Uses tables in Section 3 of this SP with a "P" symbol.

During the plot plan review, the Director shall review applications for compliance with the development standards listed in Chapter 4.0 Development Standards. The plot plan process shall be the same as that outlined in Section 18.30 of the ZC.

The following additional applications may not be identified as “Principle Permitted Uses” or “Accessory Permitted Uses” in Section 4 Design Guidelines, but they are considered ancillary to the inherent land uses. The following applications may be submitted in conjunction with the plot plan application, and may be approved or conditionally approved by the Director through approval of said plot plan:

- ❖ Park design and architecture;
- ❖ Landscape plans and selected landscape materials for all open space areas;
- ❖ Entry monumentation; and
- ❖ Private property landscape plans and selected landscape materials.

### **5.3.4 CONDITIONAL USE PERMITS**

Conditional use permits allow the City to consider special uses that are not allowed as a matter of right within a zoning district, therefore providing flexibility within a zoning ordinance.

Consideration of a conditional use permit is a discretionary action. Uses requiring a conditional use permit shall be the same as those identified with a “C” symbol on Table 3.2 of this SP. In addition, uses requiring a conditional use permit shall be subject to the filing, required findings, notification, hearing and appeal procedures identified in Section 18.28 of the ZC.

### **5.3.5 ARCHITECTURAL REVIEW**

This SP provides builders and developers with flexibility with respect to architectural styles and provides the flexibility to incorporate a wide range of complementary building designs and architectural styles. To ensure the creation of a high quality development that exhibits cohesive community character and complementary building design, all applications for a plot plan, public use permit, or conditional use permit shall be subject to the architectural review process.

An application for architectural review shall be filed with the Planning Division in a manner prescribed by the Director, including, but not limited to: plans, elevations, and materials and color boards. The Director will review all development applications and ensure the proposed project meets the intent of the development standards and design guidelines.

The decision of the Director shall be final and effective 14 days after a written determination has been made unless, within said time, a written appeal to the Planning Commission is filed by the applicant, property owners subject to the architectural review, or by any member of the City Council or Planning Commission. Appeals shall be undertaken in compliance with the procedures outlined in the ZC.

The Director may refer any item to the Planning Commission at his or her discretion.

## **5.4 FINANCING**

Various techniques are available for financing the required improvements. A detailed financing plan should be prepared in order to successfully implement the improvements and programs proposed by this SP. Along with establishing specific goals and policies, the financing plan should analyze a series of methods to finance infrastructure and other improvements, recommend preferred alternatives, and develop a process for enacting financing methods.

The appropriate mechanism for each particular improvement shall be tied to the phasing, established conditions of approval and site plan/design review approval. The following is a summary of possible methods that could be used to finance SP improvements. There may be other sources available to finance improvement projects, such as government grants, or various types of bonds not listed below.

### **5.4.1 FINANCING PLAN**

The developer or builder shall be responsible for financing construction of the infrastructure improvements required to support the development, such as perimeter and internal streets, water lines, sewers, and storm drains. All necessary infrastructure improvements shall be developed in conjunction with the approved phasing plan. The financing of construction, operation, and maintenance of public improvements and facilities will include funding through a combination of financing mechanisms. However, the developer or builder shall be ultimately responsible for all fair share costs associated with implementing the development, including but not limited to the costs of providing infrastructure and complying with mitigation measures, conditions of approval, and other requirements of the development.

Financing may involve a combination of impact fees and exactions, special assessment districts, landscaping and lighting districts, and other mechanisms agreed to by the developer and the City as noted below. Developer or builder-

funded improvements may be subject to a reimbursement agreement or credits against fees pursuant to provisions of a development agreement or conditions of approval. The City and developer or builder will cooperate to ensure that the public facilities are built in accordance with all requirements of the SP and EIR. A development agreement and conditions of approval may be used to facilitate this process.

### **5.4.2 DEVELOPER FUNDING**

In many cases, certain on-site facilities are tied directly to individual projects. In these cases, it is reasonable to expect the developer, builder or property owner to pay the entire cost of the facility in order to secure development rights. On-site local streets, utility connections from main trunk lines, and drainage facilities are good examples of facilities that are normally required concurrent with development of an individual parcel funded by the developer or builder.

### **5.4.3 SPECIAL ASSESSMENT DISTRICTS**

A special assessment district is a type of benefit district that requires a vote by the property owners to encompass a defined and limited geographic area. The City or other agencies may form a special assessment district under one of several different statutory acts to construct public improvements such as streets, storm drains, sidewalks, streetlights, sewers, parks landscape, and other similar capital facilities. The special assessment districts can issue bonds to finance those improvements and levy a special assessment to pay debt service on those bonds.

A special assessment district may fund improvements within the entire SP area or smaller sub areas where special improvements are constructed that directly benefit only certain property owners. Special assessments districts may only be used to pay for projects that are of specific and direct benefit to the property owner being assessed. The amount of the assessment must directly relate to the amount of benefit received by the property owner.



#### **5.4.4 LANDSCAPING AND LIGHTING DISTRICTS**

Landscape and Lighting Districts (LLDs) may be used for maintenance and servicing of landscaping and lighting through annual assessments on benefiting properties. LLDs may also provide for maintenance of appurtenant features, including curbs, gutters, walls, sidewalks or paving, and irrigation or drainage facilities.

#### **5.4.5 COMMUNITY FACILITIES DISTRICTS AND MELLO-ROOS**

The Mello-Roos Community Facilities Act of 1982 allows the creation of special districts authorized to levy a special tax and issue tax exempt bonds to finance public facilities and services. A Community Facilities District (CFD) may be initiated by the legislative body or by property owner petition and must be approved by a 2/3 majority of property owners or registered voters (if there are more than 12 registered voters living in the area). Because there is no requirement to show special benefit, Mello-Roos levies may be used to fund improvements of general benefit, such as fire and police facilities, libraries, and parks, as well as improvements that benefit specific properties. The provision also allows the reallocation of cost burdens to alleviate untenable burdens on specific properties.

#### **5.4.6 OTHER FUNDING SOURCES**

Other sources may be available to finance improvement projects, such as government grants, private developer cost sharing agreements, or various types of bonds not listed above.

### **5.5 MAINTENANCE PLAN**

Maintenance of open space areas, recreational facilities, and major roadway landscaping is of utmost importance to the performance and appearance of Fleming Ranch. Therefore, a comprehensive maintenance plan will be established for standards as well as guidance for the upkeep and governance of public common areas within the SP area. Refer to Figure 5.1: Maintenance Diagram on page 5-9.

#### **5.5.1 APPORTIONMENT OF COSTS FOR MAINTENANCE OF COMMON AREAS**

In order to ensure timely commencement and sufficient funding for maintenance of public facilities and common areas, the SP will annex into an existing maintenance organization, city-wide CFD, or create an active management organization such as a community-wide maintenance district or a neighborhood homeowners association (HOA). This maintenance district will be empowered to apportion costs for shared public facilities and common area maintenance within the SP and/or respective phases of the SP.

#### **5.5.2 COMMON AREA MAINTENANCE**

Common areas such as pocket parks, neighborhood parks, water quality basins, open space areas and landscaped areas are identified in the SP as being available for the benefit of all residents of the SP area and to the public. Such common areas shall be maintained either by a public/private entity such as a LLD or CFD, or by an association which includes as its participating owners all property within the SP, and the responsible agency shall assume maintenance responsibility for such area. The public parks shall be maintained by a public agency or public maintenance organization and not an HOA.

#### **5.5.3 SPECIFIC FACILITIES MAINTENANCE**

In residential areas of the project, smaller associations may be formed to assume ownership and maintenance responsibility for common areas and facilities that benefit only the residents in those areas. Private open space areas and private roadways are examples of facilities that could come under the jurisdiction of a neighborhood HOA.

#### **5.5.4 ROADWAYS AND ROADWAY LANDSCAPING**

Development in the SP area may annex into a Citywide maintenance CFD (in lieu of the LLDs) to provide maintenance services to certain approved public improvements. All public SP roadways will be designed and constructed to standards stated in this SP and will, therefore, be entered into the City system of roads for operation and maintenance as approved by the City Council. Any private roads or accesses will be maintained by an association or other public/private entity, as described above.

Roadway landscaping within the right-of-way (such as the Enhanced Paseos), landscaping within the raised medians, and any hardscape outside of any roadway right-of-way, shall be maintained by a public/private entity or other master association.

#### **5.5.5 PRIVATE AREA MAINTENANCE**

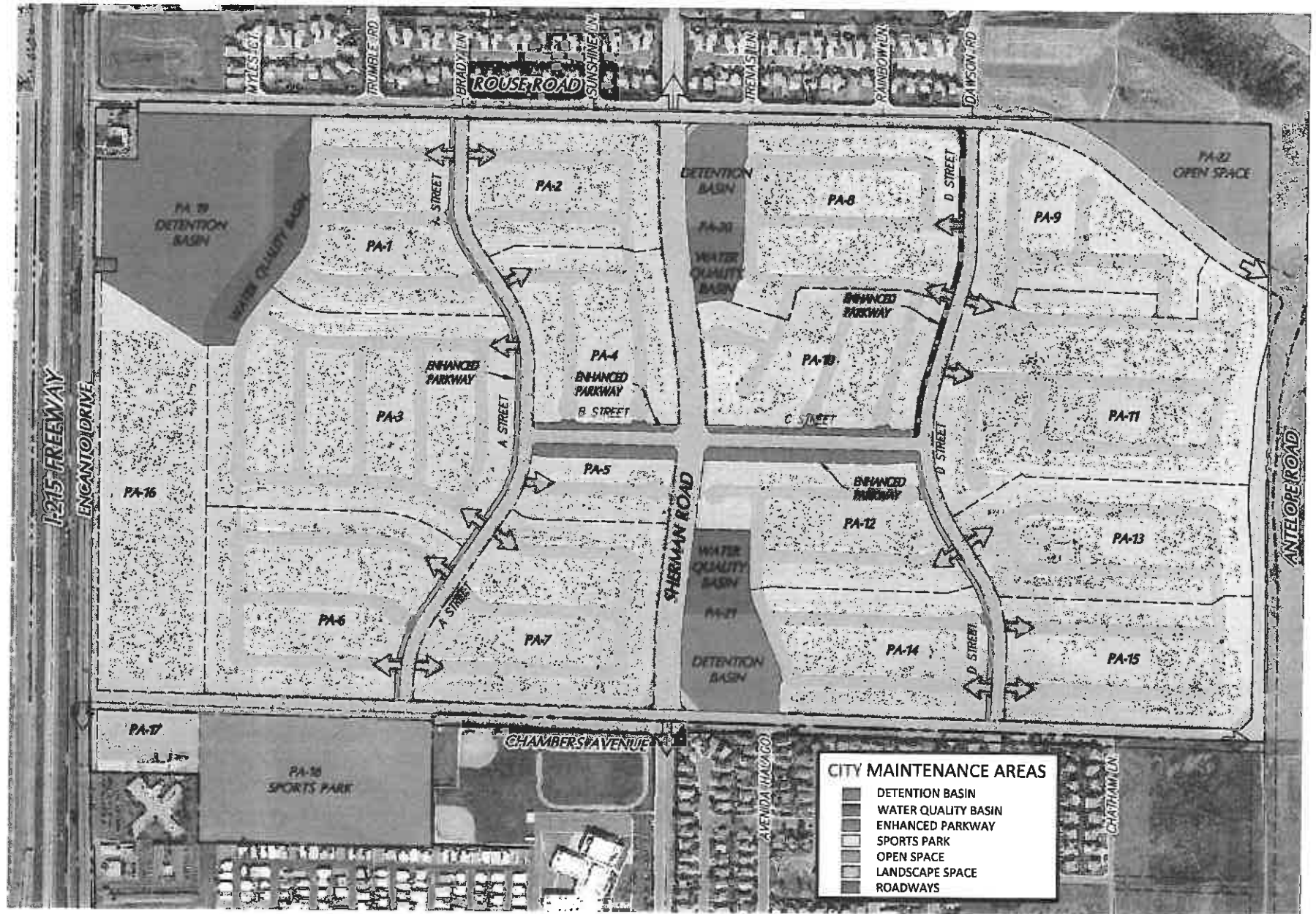
Front yard setback areas which are open to the street shall be maintained by the homeowner or property owner. Also, sloped areas in rear, side and front yards will be maintained by the homeowner or property owner.

### **5.6 SUMMARY OF FINANCING & MAINTENANCE OPTIONS**

The financing and maintenance plan for the SP will ensure the timely completion of public facilities, utilities and other necessary capital improvements as well as the proper maintenance of these facilities. Table 5.1: Financing and Maintenance Plan Summary indicates the parties responsible for construction, financing, and maintaining the public improvements proposed by the SP.

# SECTION 5

## ADMINISTRATION & IMPLEMENTATION



**FIGURE 5.1: MAINTENANCE DIAGRAM**

**TABLE 5.1: FINANCING AND MAINTENANCE PLAN SUMMARY**

SERVICE OR FACILITY	PARTY(IES) EXECUTING CONSTRUCTION	PARTY(IES) FINANCING CONSTRUCTION	PARTY(IES) RESPONSIBLE FOR OPERATION AND MAINTENANCE
<b>ROADWAY ELEMENTS</b>			
Public Streets (including Parkways)	Master Developer	Master Developer or CFD	City of Menifee, LLD or CFD
Public Street Medians	Master Developer	Master Developer or CFD	City of Menifee, LLD or CFD
Private Streets and Sidewalks (if applicable)	Master Developer	Master Developer	HOA
<b>PUBLIC FACILITIES</b>			
Storm Drainage Facilities	Master Developer	Master Developer or CFD	Riverside County Flood Control & Water Conservation District/City of Menifee
Detention/Water Quality Basin	Master Developer	Master Developer or CFD	City of Menifee, LLD or CFD/RCFC & WCD
Sewer Facilities	Master Developer	Master Developer or CFD	Eastern Municipal Water District
On-Site Water Facilities	Master Developer	Master Developer or CFD	Eastern Municipal Water District
Off-Site Water Facilities	Master Developer	Master Developer or CFD	Eastern Municipal Water District
Natural Open Space	Master Developer	Master Developer or CFD	Conservation agency or equivalent city-wide entity
Community Park	TBD	TBD	City of Menifee, CFD
<b>SEMI-PRIVATE FACILITIES</b>			
Common Area Landscape & Improvements/ Private Parks	Master Developer	Master Developer	HOA, LLD or City CFD
Private Recreation Centers	Master Developer	Master Developer	HOA, LLD or City CFD
Landscaped Paseos	Master Developer	Master Developer	HOA, LLD or City CFD
Community Walls/Fences/Entry Gates	Master Developer	Master Developer	HOA, LLD or City CFD
Privacy Fences	Master Developer	Builder	Homeowner
Master Plan Signage	Master Developer	Master Developer	HOA, LLD or City CFD
Neighborhood Signage	Builder	Builder	HOA, LLD or City CFD
<b>PRIVATE FACILITIES</b>			
Front Yard Landscape	Builder	Builder	Homeowner/Property Owner
Rear Yard Landscape	Homeowner/Property Owner	Homeowner/Property Owner	Homeowner /Property Owner
Commercial Signs/Landscape	Property Owner	Property Owner	Property Owner

# APPENDIX A



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## COMMUNITY PLANT PALETTE

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## A.1 COMMUNITY PLANT PALETTE

The following table outlines plant material suitable for the community of Fleming Ranch. All plants selected from Table A: Community Plant Palette shall be submitted to the City of Menifee for approval prior to the approval of final landscape construction documents.

**TABLE A: COMMUNITY PLANT PALETTE**

Potential Name	Common Name	Water Factor	Entries	Parkways	Sports Park	Homes	Retention Area Slopes
<b>Trees</b>							
<i>Acacia aneura</i>	Mulga	L/L	•	•	•	•	
<i>Acacia farnesiana</i>	Sweet Acacia	L/L	•	•	•	•	
<i>Aesculus californica</i> 'Canyon Pink'	Canyon Pink California Buckeye	L/L		•	•	•	•
<i>Albizia julibrissin</i> 'Rosea'	Silk Tree, Mimosa	M				•	
<i>Arbutus unedo</i>	Strawberry Tree	M	•			•	
<i>Brachychiton populneus</i>	Kurrajong Bottle Tree	M/L		•		•	
<i>Callistemon citrinus</i>	Lemon Bottlebrush	M/L	•	•	•	•	
<i>Casuarina cunninghamiana</i>	River She-Oak	M/L				•	
<i>Cedrus deodara</i> 'Aurea'	Aurea Deodar Cedar	M/L	•			•	
<i>Cercis occidentalis</i>	Western Redbud	V/L	•	•	•	•	
<i>Chilopsis linearis</i>	Desert Willow	M/L	•	•	•	•	•
<i>Chitalpa tashkentensis</i> 'Pink Dawn'	Pink Dawn Chitalpa	M/L				•	
<i>Chorisia speciosa</i>	Floss Silk Tree	M		•	•		
<i>Cupressus arizonica</i> 'Blue Ice'	Blue Ice Arizona Cypress	L/L				•	

**TABLE A: COMMUNITY PLANT PALETTE**

Botanical Name	Common Name	Water Factor	Entries	Parkways	Sports Park	Homes	Detention Area Slopes
<i>Cupressus arizonica</i> 'Compacta'	Compact Arizona Cypress	LVL	•			•	
<i>Cupressus sempervirens</i>	Italian Cypress	LVL	•			•	
<i>Eriobotrya deflexa</i>	Bronze Loquat	M	•			•	
<i>Erythrina coralloides</i>	Naked Coral Tree, Flame Coral Tree	M/L	•	•		•	
<i>Fraxinus angustifolia</i> 'Raywood'	Raywood Ash	M	•	•	•		
<i>Fraxinus greggi</i>	Little Leaf	M	•	•			•
<i>Fraxinus uhdei</i>	Shamel Ash	M	•	•	•	•	
<i>Fraxinus velutina</i> 'Modesto'	Modesto Ash	M	•	•	•	•	•
<i>Gleditsia triacanthos</i> 'Shademaster'	Shademaster Honey Locust	M	•			•	
<i>Havardia mexicana</i>	Mexican Ebony (Desert Ironwood)	LVL				•	
<i>Jacaranda mimosifolia</i>	Jacaranda	M		•	•		
<i>Juniperus scopulorum</i> 'Tolleson's Weeping'	Tolleson's Weeping Juniper	M/L				•	
<i>Koelreuteria bipinnata</i>	Chinese Flame Tree	M		•	•		

**TABLE A: COMMUNITY PLANT PALETTE**

Botanical Name	Common Name	Water Factor	Entries	Parkways	Sports Park	Homes	Retention Area Slopes
<i>Lagerstroemia indica</i> 'Muskogee'	Muskogee Crape Myrtle	M	•	•	•	•	
<i>Lagerstroemia indica</i> 'Natchez'	Natchez Crape Myrtle	M	•	•	•	•	
<i>Lagerstroemia indica</i> 'Tuscarora'	Tuscarora Crape Myrtle	M	•	•	•	•	
<i>Laurus nobilis</i> 'saratoga'	Sweet Bay	M/L	•	•	•	•	
<i>Lithocarpus densiflorus</i>	Tanbark Oak, Tan Oak	L/VL				•	
<i>Melaleuca linarifolia</i>	Flax Leaf Paperbark	L		•	•	•	
<i>Ulmus parvifolia</i>	Chinese or Evergreen Elm	M		•	•		
<i>Olea europaea</i> 'Swan Hill'	Fruitless Olive	M/L	•	•		•	
<i>Olneya tesota</i>	Desert Ironwood	M/L				•	
<i>Parkinsonia florida</i> ( <i>Cercidium floridum</i> )	Blue Palo Verde	M/L	•	•		•	
<i>Parkinsonia microphylla</i>	Foothills Palo Verde	M/L	•	•		•	
<i>Parkinsonia praecox</i>	Sonoran Palo Verde	M/L	•	•		•	•
<i>Parkinsonia x</i> 'Desert Museum' ( <i>Cercidium x Desert Museum</i> )	Desert Museum Palo Verde	M/L	•	•		•	•



# APPENDIX A

## COMMUNITY PLANT PALETTE

**TABLE A: COMMUNITY PLANT PALETTE**

Botanical Name	Common Name	Water Factor	Entries	Parkways	Sports Park	Homes	Detention Area Slopes
<i>Pistacia chinensis</i> 'Red Push'	Red Push Chinese Pistache	M	•	•		•	
<i>Pithecellobium flexicavle</i>	Texas Ebony	LVL		•		•	•
<i>Platanus racemosa</i>	California Sycamore	M/H					•
<i>Populus fremontii</i>	Western Cottonwood	H					•
<i>Prosopis glandulosa</i> thornless 'AZT'	Thornless Honey Mesquite 'AZT'	L	•	•		•	•
<i>Prosopis glandulosa</i> v. <i>glandulosa</i>	Texas Honey Mesquite	L	•	•		•	•
<i>Prosopis juliflora</i>	Arizona Mesquite	L	•	•		•	•
<i>Prosopis x</i> 'Phoenix'	Phoenix Mesquite	M/L	•		•	•	•
<i>Prunus caroliniana</i>	Carolina Laurel Cherry	M	•	•		•	
<i>Prunus ilicifolia</i>	Hollyleaf Cherry	M/L	•		•	•	
<i>Prunus ilicifolia</i> <i>lyonii</i>	Catalina Cherry	M/L	•		•	•	
<i>Quercus lobata</i>	Valley Oak	M/L		•	•		•
<i>Quercus virginiana</i>	Southern Live Oak	M/L		•	•		•
<i>Quercus illex</i>	Holly Oak	M/L		•	•		•
<i>Quercus suber</i>	Cork Oak	M/L		•	•		•
<i>Rhus lancea</i>	African Sumac	M/L				•	

**TABLE A: COMMUNITY PLANT PALETTE**

Botanical Name	Common Name	Water Factor	Entries	Parkways	Sports Park	Homes	Detention Area Slopes
<b>Shrubs</b>							
<i>Arctostaphylos densiflora</i>	Sonoma Manzanita	M/L	•	•	•	•	
<i>Artemisia arborescens</i>	Silver Wormwood	L/VL			•	•	•
<i>Atriplex hymenelytra</i>	Desert Holly	VL	•	•		•	•
<i>Atriplex lentiformis</i>	Quail Bush	L/VL	•	•	•	•	
<i>Atriplex lentiformis breweri</i>	Brewer Salt Bush	L/VL	•	•	•	•	
<i>Baccharis hybrid 'Starn'</i>	Thompson Baccharis	L			•	•	
<i>Baccharis pilularis</i>	Coyote Brush	L/VL			•	•	
<i>Baccharis sarathroides</i>	Desert Broom	L/VL			•	•	
<i>Berberis thunbergii 'Rose Glow'</i>	Rose Glow Barberry	L	•	•	•	•	•
<i>Buddleja marrubiifolia</i>	Wooly Butterfly Bush	L	•	•		•	
<i>Buxus Sempervirenex</i>	Common Boxwood	M	•	•		•	
<i>Calycanthus occidentalis</i>	Spice Bush	L/M	•	•	•	•	
<i>Carpenteria californica</i>	Bush Anemone	M/L	•	•	•	•	•
<i>Ceanothus species</i>	California Wild Lilac	L	•	•	•	•	•
<i>Cordia parvifolia</i>	Little-Leaf Cordia	L		•	•	•	

# APPENDIX A

## COMMUNITY PLANT PALETTE

**TABLE A: COMMUNITY PLANT PALETTE**

Botanical Name	Common Name	Water Factor	Entries	Parkways	Sports Park	Homes	Detention Area Slopes
<i>Coreopsis grandiflora</i>	Summer Coreopsis	M		•	•	•	
<i>Correa species</i>	Australian Fuchsia	M/L		•	•	•	
<i>Cotoneaster buxifolius</i>	Cotoneaster Buxifolius	L/M	•	•	•	•	
<i>Cotoneaster congestus</i>	Pyrenees Cotoneaster	L/M	•	•	•	•	
<i>Cotoneaster glaucophyllus</i>	Bright Bead Cotoneaster	M	•	•	•	•	
<i>Elaeagnus pungens</i>	Silverberry silverberry	L	•	•	•	•	
<i>Encelia californica</i>	Brown Eyed Susan	L/VL			•	•	
<i>Encelia farinosa</i>	Brittlebush	L/VL			•	•	
<i>Eremophila maculata</i>	Red Eremophila	L				•	
<i>Eriogonum fasciculatum</i>	California Buckwheat	L	•	•	•	•	
<i>Escallonia species</i>	Escallonia	M		•	•	•	
<i>Euonymus japonicus sp</i>	Euonymus	M/L		•	•	•	
<i>Galvezia speciosa</i>	Island Bush Snapdragon	L/VL	•	•	•	•	
<i>Grevillea 'Noellii'</i>	Noel's Grevillia	L	•	•	•	•	
<i>Hebe 'Veronica Lake'</i>	Veronica Lake Hebe	M	•	•	•	•	
<i>Heteromeles arbutifolia</i>	Toyon	VL	•	•	•	•	
<i>Ilex corunta 'Burfordii'</i>	Burford Holly	M		•	•	•	
<i>Ilex vomitoria</i>	Yaupon	M/ L				•	

**TABLE A: COMMUNITY PLANT PALETTE**

Botanical Name	Common Name	Water Factor	Entries	Parkways	Sports Park	Homes	Detention Area Slopes
<i>Lantana camara</i>	Bush Lantana	L	•	•		•	
<i>Lantana montevidensis</i> (gold cultivars)	Trailing Lantana	L	•	•		•	
<i>Larrea tridentata</i>	Creosote Bush	VL	•	•		•	
<i>Lavandula species</i>	Lavender	L	•	•		•	
<i>Lavatera assurgentiflora</i>	Tree Mallow	L	•	•	•	•	
<i>Lavatera bicolor</i>	Carolina Tree Mallow	L/M	•	•		•	
<i>Leptospermum laevigatum</i>	Australian Tea Tree	L		•		•	
<i>Leucophyllum species candidum</i>	Texas Sage	L	•	•	•	•	•
<i>Ligustrum japonicum</i> 'Texanum'	Texas Privet	M	•	•	•	•	
<i>Lobelia laxiflora</i>	Mexican Bush Lobelia	VL	•	•	•	•	
<i>Lonicera nitida</i>	Box Honeysuckle	M/L	•	•	•	•	•
<i>Lycium fremontii</i>	Wolfberry	L		•		•	•
<i>Mahonia species</i>	Oregon Grape	M	•	•	•	•	
<i>Melaleuca nesophila</i>	Pink Melaleuca	L	•	•	•	•	
<i>Mimulus aurantiacus</i>	Sticky Monkey Flower	L	•	•	•	•	
<i>Myrtus communis</i> 'Variegata'	Variegated Common Myrtle	M/L	•	•	•	•	

**APPENDIX A**  
COMMUNITY PLANT PALETTE

**TABLE A: COMMUNITY PLANT PALETTE**

Botanical Name	Common Name	Water Factor	Entries	Parkways	Sports Park	Homes	Detention Area Slopes
<i>Nandina domestica</i> species	Heavenly Bamboo	M/L	•	•	•	•	
<i>Phlomis fruticosa</i>	Jerusalem Sage	M/L	•	•	•	•	•
<i>Photinia x fraseri</i>	Fraser's Photina	M				•	
<i>Pittosporum tobira</i> and hybrids	Japanese Mock Orange	M	•	•	•	•	
<i>Prunus ilicifolia</i>	Hollyleaf Cherry	LVL	•	•		•	
<i>Pyracantha</i> species	Firethorn	M/L	•	•		•	
<i>Rhamnus californica</i>	Coffeeberry	LVL			•	•	•
<i>Rhamnus crocea</i>	Redberry	LVL					•
<i>Raphiolepis umbellata</i> 'Minor'	White Compact Yeddo Hawthorn	M/L	•	•	•	•	
<i>Ribes sanguineum</i>	Red Flowering Currant	M/L	•	•	•	•	
<i>Ribes viburnifolium</i>	Evergreen Currant	M/L	•	•	•	•	
<i>Rosa banksiae</i>	Lady Bank's Rose	M/L	•	•		•	
<i>Rosa californica</i>	California Wild Rose	L	•	•		•	•
<i>Rosmarinus officinalis</i> 'Tuscan Blue'	Tuscan Blue Rosemary	M/L	•	•	•	•	
<i>Ruellia brittoniana</i>	Mexican Barrio Ruellia	LVL			•	•	
<i>Ruellia californica</i>	Sonoran Desert Ruellia	LVL			•	•	

**TABLE A: COMMUNITY PLANT PALETTE**

Botanical Name	Common Name	Water Factor	Entries	Parkways	Sports Park	Homes	Detention Area Slopes
<i>Santolina rosmarinifolia</i>	NCN	L			•	•	
<i>Spiraea douglasii</i>	Western Spiraea	M	•	•	•	•	
<i>Teucrium fruticans</i>	Bush Germander	M/L	•		•	•	
<i>Viburnum japonicum</i>	Viburnum	M	•	•	•	•	
<i>Viburnum suspensum</i>	Sandkwa Viburnum	M	•	•	•	•	
<i>Viguiera deltoidea</i>	Golden Eye	LVL	•	•		•	
<i>Westringia fruticosa</i>	Coast Rosemary	LVL	•	•	•	•	•
<i>Westringia</i> 'Wynyabbie Gem' Gem*	Wynyabbie Gem Westringia	M/L	•	•			•
<i>Xylosma congestum</i>	Shiny Xylosma	M/L	•	•		•	

**TABLE A: COMMUNITY PLANT PALETTE**

Botanical Name	Common Name	Water Factor	Entries	Parkways	Sports Park	Homes	Detention Area Slopes
<b>Groundcover</b>							
<i>Adenostoma fasciculatum</i> var. <i>prostratum</i>	Prostrate Chamise	M/L			•	•	•
<i>Arctotheca calendula</i>	Cape Gold, Cape Weed	M/L		•	•	•	•
<i>Atioplex semibaccata</i>	Creeping Saltbush	LVL	•	•	•	•	•
<i>Baccharis pilularis</i> 'Pigeon Point'	Dwarf Coyote Bush	LVL	•	•	•	•	•

# APPENDIX A

## COMMUNITY PLANT PALETTE

**TABLE A: COMMUNITY PLANT PALETTE**

Botanical Name	Common Name	Water Factor	Entries	Parkways	Sports Park	Homes	Retention Area Slopes
<i>Ceanothus griseus horizontalis</i>	Carmel Ceanothus	M/L	•	•	•	•	
<i>Cistus corbariensis</i>	White Rockrose	M/L	•	•	•	•	•
<i>Coprosma kirkii</i>	Coprosma	M		•		•	•
<i>Dalea greggi</i>	Trailing Indigo Bush	M/L			•	•	
<i>Drosanthemum floribundum</i>	Rosea Ice Plant	L/VL		•	•	•	
<i>Euphorbia rigida</i>	Gopher Plant	L/VL	•	•	•	•	
<i>Juniper</i> (compact varieties)	Juniper	M/L		•	•	•	•
<i>Lupinus species</i>	Lupine	M	•	•	•	•	•
<i>Mahonia repens</i>	Creeping Mahonia	M/L	•	•		•	
<i>Myoporum 'Pacificum'</i>	Pacific Myoporum	M/L	•	•		•	•
<i>Myoporum parvifolium 'Prostratum'</i>	Prostrate Myoporum	M/L	•	•	•	•	•
<i>Pelargonium species</i>	Ivy Geranium	M	•	•	•	•	
<i>Pyracantha hybrids</i>	Firethorn Species	M/L					
<i>Rosmarinus officinalis 'Prostratus'</i>	Prostrate Rosemary	M/L	•	•	•	•	•
<i>Satureja douglasii</i>	Yerba Buena	M/L	•			•	
<i>Thymus species</i>	Thyme	M				•	
<i>Trachelospermum jasminoides</i>	Star Jasmine	M	•	•	•	•	

**TABLE A: COMMUNITY PLANT PALETTE**

Botanical Name	Common Name	Water Factor	Entries	Parkways	Sports Park	Homes	Detention Area Slopes
<i>Verbena Rigida</i>	Verbena	M/L	•			•	
<i>Zauchneria californica</i>	California Fuschia	M/L	•			•	

**TABLE A: COMMUNITY PLANT PALETTE**

Botanical Name	Common Name	Water Factor	Entries	Parkways	Sports Park	Homes	Detention Area Slopes
<b>Vines</b>							
<i>Bougainvillea species</i>	Bougainvillea	LVL	•	•		•	
<i>Campsis radicans</i>	Common Trumpet Creeper	M/L		•	•	•	
<i>Cissus trifoliata</i>	Native Grape Ivy	L	•			•	
<i>Clematis armandii</i>	Evergreen Clematis	M	•			•	
<i>Gelsemium sempervirens</i>	Carolina Jasmine	M/L	•			•	
<i>Lonicera Japonica</i>	Japanese Honeysuckle	M/L	•		•	•	
<i>Mandevilla hybrida</i>	Mandevilla	M		•		•	
<i>Macagnia lilacina</i>	Lavendar Orchid Vine	M				•	
<i>Podranea ricasoliana</i>	Pink Trumpet Vine	M	•			•	
<i>Vitis californica</i>	California Wild Grape	M/L				•	
<i>Vitis girdiana</i>	Desert Grape	M/L	•	•		•	
<i>Wisteria floribunda</i>	Japanese Wisteria	M	•		•	•	



**TABLE A: COMMUNITY PLANT PALETTE**

Botanical Name	Common Name	Water Factor	Entries	Parkways	Sports Park	Homes	Retention Area Slopes
<b>Grasses</b>							
<i>Carex tumulicola</i>	Berkeley Sedge	M	•	•	•	•	•
<i>Deschampsia caespitosa</i>	Tufted Hair Grass	M	•	•	•	•	•
<i>Distichlis spicata</i> 'Stricata'	Salt Grass	M	•	•	•	•	•
<i>Eleocharis macrostachya</i>	Spike Rush	H					•
<i>Festuca glauca</i>	Blue Fescue	M	•	•	•	•	
<i>Juncus acutus</i>	Spiny Rush	H					•
<i>Juncus patens</i>	California Gray Rush	H					•
<i>Leymus condensatus</i>	Giant Wild Rye	M/L					
<i>Leymus triticoides</i>	Creeping Wild Rye	M	•	•	•	•	•
<i>Lilium parvalinum</i>	Leopard Lily	M	•	•	•	•	•
<i>Miscanthus sinensis</i> 'Little Kitten'	Dwarf Silver Eulalia Grass	H/M	•	•	•	•	•
<i>Muhlenbergia capillaris</i>	Pink Muhly	M/L	•	•	•	•	•
<i>Muhlenbergia emersleyi</i>	Bull Grass	M/L	•	•	•	•	•
<i>Nassella tenuissima</i>	Mexican Feather Grass	L/VL	•	•	•	•	•
<i>Panicum virgatum</i> 'Northwind'*	Northwind Switch Grass	M	•	•	•	•	•
<i>Sorghastrum avenaceum</i>	Indian or Wood Grass	M	•	•	•	•	•

**TABLE A: COMMUNITY PLANT PALETTE**

Botanical Name	Common Name	Water Factor	Entries	Parkways	Sports Park	Homes	Retention Area Slopes
<i>Stipa avenaceum</i>	Needle Grass	M	•	•	•	•	•
<i>Zoysia 'De Anza'</i>	Turf Zoysia De Anza	M	•	•	•	•	•

**TABLE A: COMMUNITY PLANT PALETTE**

Botanical Name	Common Name	Water Factor	Entries	Parkways	Sports Park	Homes	Retention Area Slopes
<b>Accent Plants</b>							
<i>Agapanthus species</i>	Lily of the Nile	M		•		•	
<i>Agave species</i>	Agave	L		•	•	•	
<i>Diets bicolor</i>	Fortnight Lily	M/L	•	•	•	•	
<i>Echeveria elegans</i>	Hens and Chickens	L		•		•	
<i>Euphorbia millii</i>	Crown of Thorns	L	•	•		•	
<i>Fouquieria spendens</i>	Ocotillo	LVL	•	•	•	•	
<i>Hemerocallis hybrids</i>	Day Lily	M	•	•		•	
<i>Iris douglasiana</i>	Douglas Iris	M/L	•	•		•	
<i>Senecio cineraria</i>	Dusty Miller	L	•	•		•	
<i>Senecio mandraliscae</i>	Blue Chalk Sticks	M		•		•	
<i>Trichostema lanatum</i>	Woolly Blue Curls	LVL	•	•		•	
<i>Tulbaghia violacea</i>	Society Garlic	M		•		•	
<i>Yucca species</i>	Yucca, Joshua Tree	L		•			

PAGE BREAK



**AIRPORT LAND USE COMMISSION  
RIVERSIDE COUNTY**



October 26, 2017

**CHAIR**  
Rod Ballance  
Riverside

**VICE CHAIRMAN**  
Steve Manos  
Lake Elsinore

**COMMISSIONERS**

Arthur Butler  
Riverside

John Lyon  
Riverside

Glen Holmes  
Hemet

Russell Betts  
Desert Hot Springs

Steven Stewart  
Palm Springs

**STAFF**

**Director**  
Simon A. Housman

John Guerin  
Paul Rull  
Barbara Santos

County Administrative Center  
4080 Lemon St., 14th Floor  
Riverside, CA 92501  
(951) 955-5132

[www.rcaluc.org](http://www.rcaluc.org)

Mr. Sergio Gutierrez, Project Planner  
City of Moreno Valley Planning Department  
14177 Frederick Street  
Moreno Valley CA 92553

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –  
DIRECTOR'S DETERMINATION**

File No.: ZAP1288MA17 – Letter 1 of 2  
Related File No.: PEN17-0134 (Change of Zone)  
APNs: 485-081-037 and -043

Dear Mr. Gutierrez:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to ALUC Resolution No.15-01 (as adopted on August 13, 2015), staff reviewed City of Moreno Valley Case No. PEN17-0134 (Change of Zone), a proposal to change the zoning on 4.47 acres located southerly of John F. Kennedy Drive and westerly of Perris Boulevard from Neighborhood Commercial (NC) to Community Commercial (CC).


The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E, nonresidential intensity is not restricted.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan ("March ALUCP").

This finding of consistency relates to airport compatibility issues and does not necessarily constitute an endorsement of the proposed Change of Zone. As the site is located within Compatibility Zone E, both the existing and proposed zoning of this property are consistent with the March ALUCP.

If you have any questions, please contact Paul Rull, ALUC Urban Regional Planner IV, at (951) 955-6893 or John Guerin, ALUC Principal Planner, at (951) 955-0982.

Sincerely,  
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

  
Simon A. Housman, ALUC Director

## AIRPORT LAND USE COMMISSION

Attachments: Notice of Airport in Vicinity

cc: Gossett Development (applicant)  
Winchester Associates, David Slawson (representative)  
Professors Fund IV, LLC, Tom Bobowski (property owner)  
Gary Gosliga, Airport Manager, March Inland Port Airport Authority  
Denise Hauser or Daniel Rockholt, March Air Reserve Base  
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1288MA17\ZAP1288MA17.CZLTR.doc

**AIRPORT LAND USE COMMISSION  
RIVERSIDE COUNTY**



October 26, 2017

**CHAIR**  
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Riverside

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Mr. Sergio Gutierrez, Project Planner  
City of Moreno Valley Planning Department  
14177 Frederick Street  
Moreno Valley CA 92553

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –  
DIRECTOR’S DETERMINATION**

File No.: ZAP1288MA17 – Letter 2 of 2  
Related File No.: PEN17-0135 (Conditional Use Permit)  
APNs: 485-081-037 and -043

Dear Mr. Gutierrez:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed City of Moreno Valley Case No. PEN17-0135 (Conditional Use Permit), a proposal to construct a 90,511 square foot mini-storage facility on 4.47 acres located southerly of John F. Kennedy Drive and westerly of Perris Boulevard.

The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E, nonresidential intensity is not restricted.

The elevation of Runway 14-32 at March Air Reserve Base/Inland Port Airport is approximately 1,535 feet above mean sea level (AMSL) at its northerly terminus and 1,488 feet AMSL at its southerly terminus. At a distance of 11,848 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Services (FAA OES) review could be required for any structures with an elevation at top of roof exceeding 1,606 feet AMSL. The site’s elevation is 1,536 feet AMSL, and has a proposed maximum building height of 16 feet, resulting in a top point elevation of 1,552 feet AMSL. Therefore, FAA OES review is not required.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, provided that the City of Moreno Valley applies the following recommended conditions:

# AIRPORT LAND USE COMMISSION

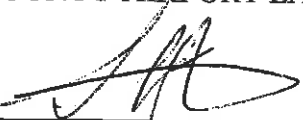
## CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site.
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
3. The landowner shall provide the attached notice to all potential purchasers of the property.
4. Any new aboveground detention or water quality basins on the site shall be designed so as to provide for a maximum 48-hour detention period following the conclusion of the storm event for the design storm (may be less, but not more), and to remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping.

If you have any questions, please contact Paul Rull, ALUC Urban Regional Planner IV, at (951) 955-6893 or John Guerin, ALUC Principal Planner, at (951) 955-0982.

**AIRPORT LAND USE COMMISSION**

Sincerely,  
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



---

Simon A. Housman, ALUC Director

Attachments: Notice of Airport in Vicinity

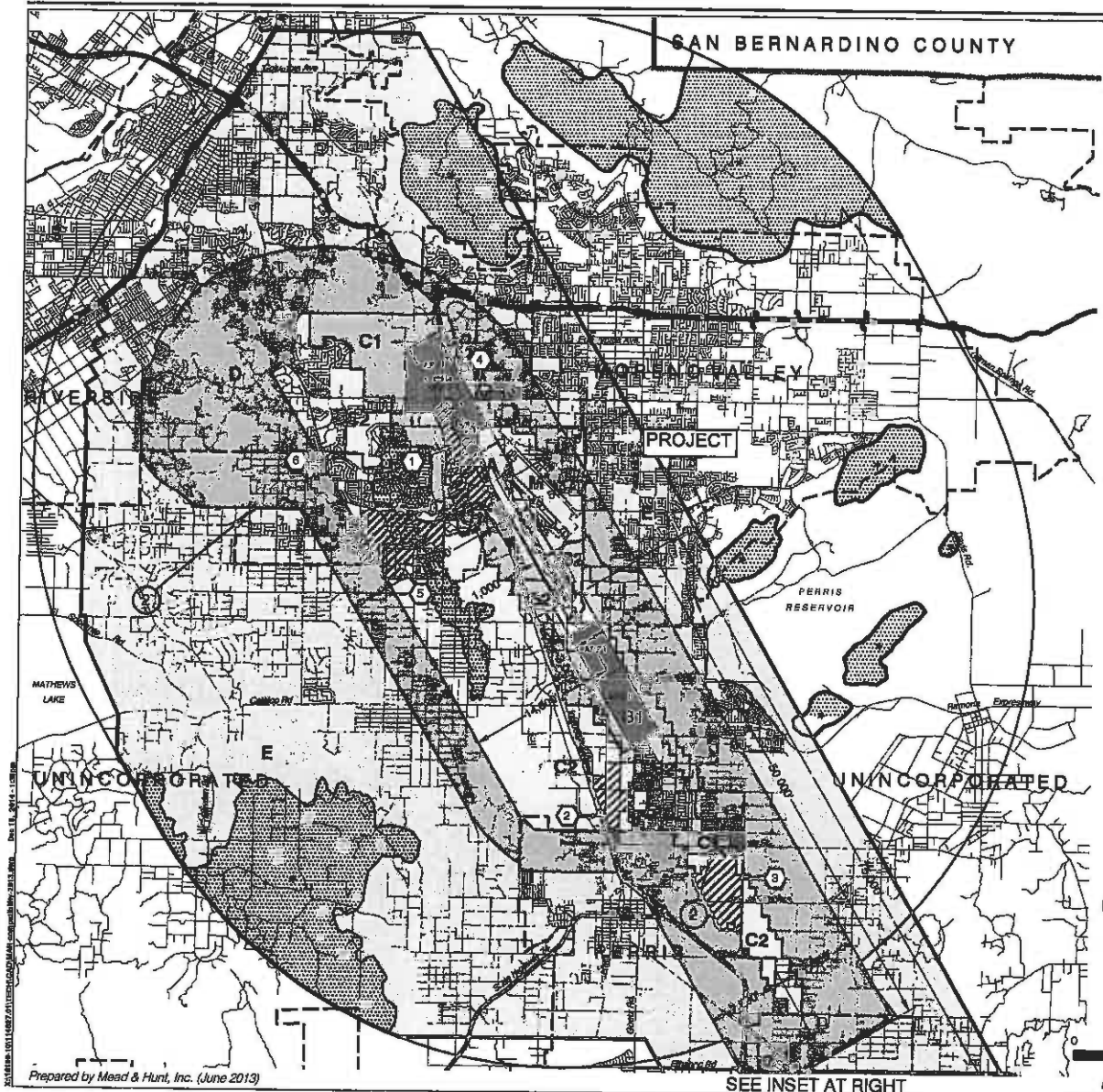
cc: Gossett Development (applicant)  
Winchester Associates, David Slawson (representative)  
Professors Fund IV, LLC, Tom Bobowski (property owner)  
Gary Gosliga, Airport Manager, March Inland Port Airport Authority  
Denise Hauser or Daniel Rockholt, March Air Reserve Base  
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1288MA17\ZAP1288MA17.CUPLTR.doc



# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



**LEGEND**

**Compatibility Zones**

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

**Boundary Lines**

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- ▨ Site-Specific Exceptions (existing local agency commitments to development projects)

① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.

② Point at which departing aircraft typically reach 3,000 feet above runway end.

③ March JPA: March Business Center/Meridian

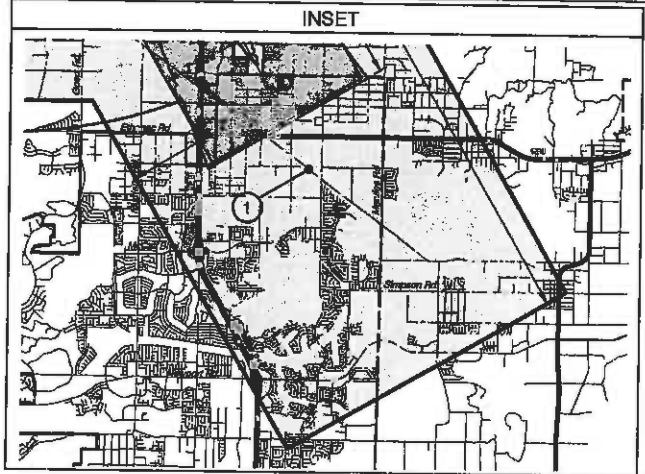
④ Perris: Harvest Landing

⑤ Perris: Park West

⑥ Moreno Valley: Affordable Housing

⑦ March JPA: Ben Clark Training Center

⑧ Riverside: Ridge Crest Subdivision



Note:  
All dimensions are measured from runway ends and centerlines.



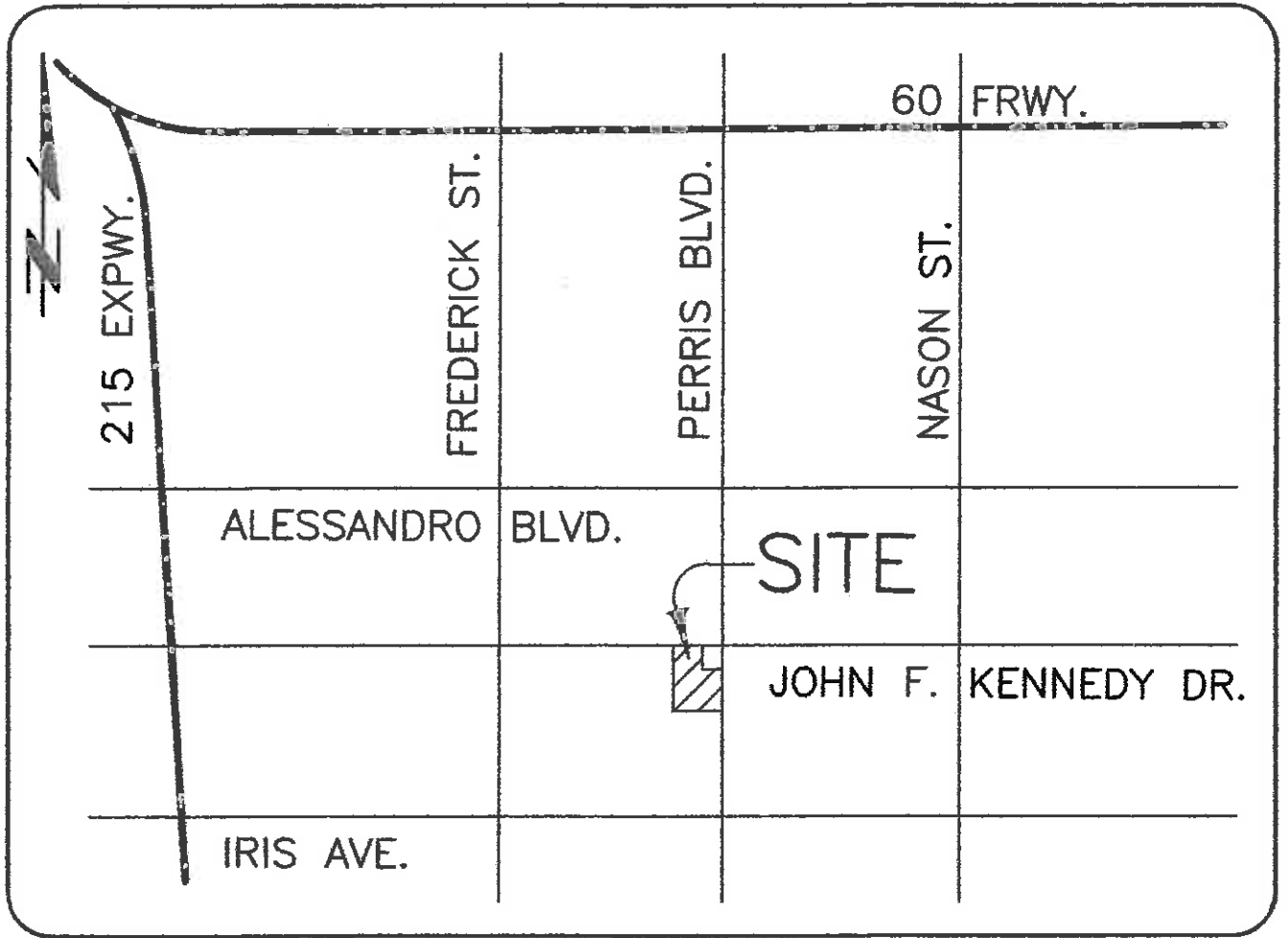
**Riverside County  
Airport Land Use Commission  
March Air Reserve Base / Inland Port Airport  
Land Use Compatibility Plan  
(Adopted November 13, 2014)**

Map MA-1  
**Compatibility Map**  
March Air Reserve Base / Inland Port Airport

Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

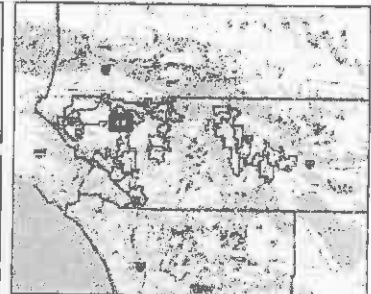
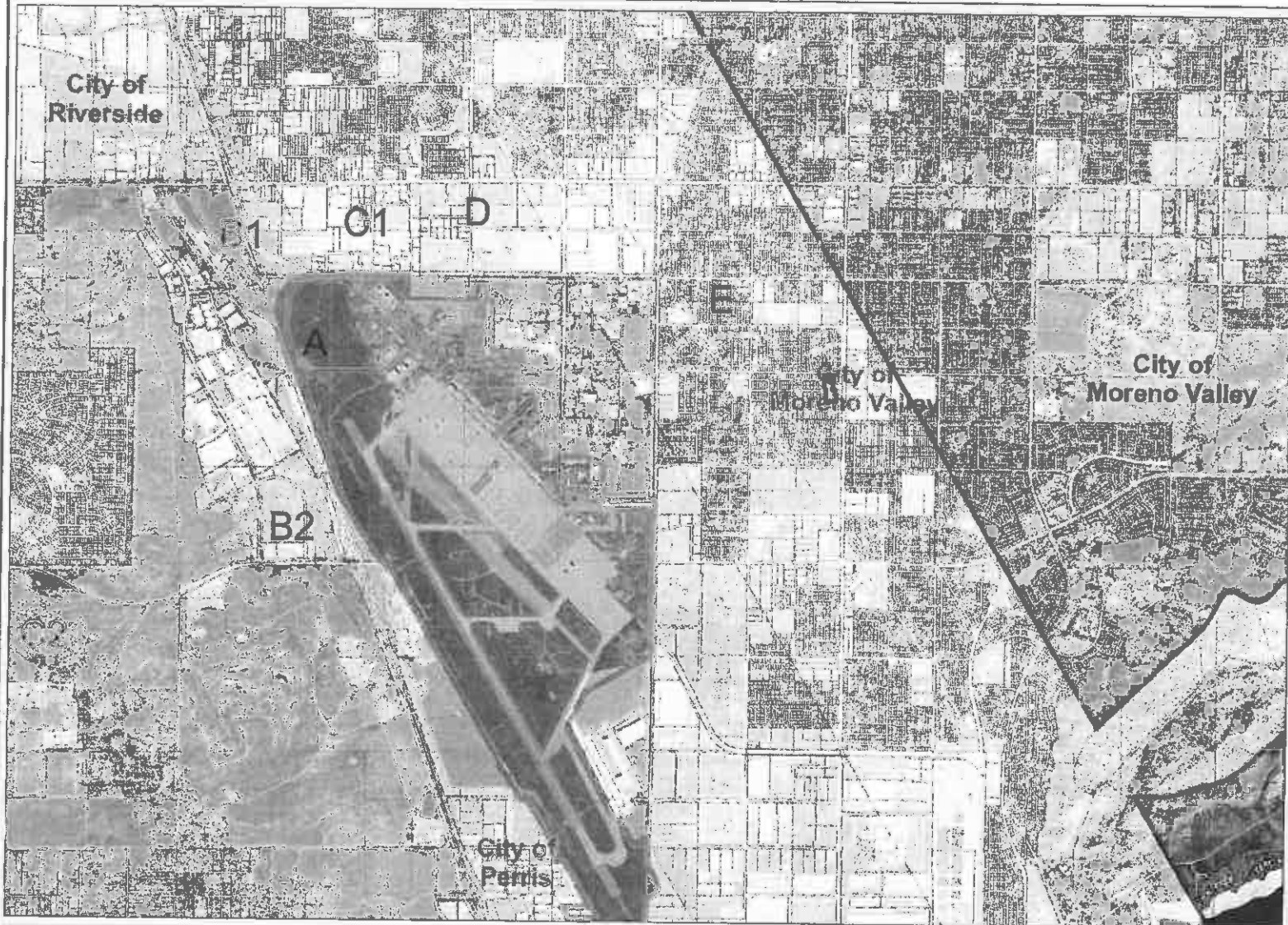
Base map source: County of Riverside 2013





























# VICINITY MAP

NTS

# My Map



## Legend

-  Airports
-  AIA
- Airport Compatibility**
-  OTHER ZONE
-  A
-  A-EXC1
-  B1
-  B1-APZ I
-  B1-APZ I-EXC1
-  B1-APZ II
-  B1-APZ II-EXC1
-  B1-EXC1
-  B2
-  B2-EXC1
-  C
-  C1
-  C1-EXC1
-  C1-EXC3
-  C1-EXC4
-  C1-HIGHT
-  C2
-  C2-EXC1
-  C2-EXC2
-  C2-EXC3
-  C2-EXC5
-  C2-EXC6
-  C2-HIGHT



0 5,165 10,331 Feet



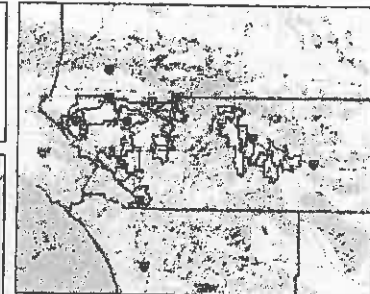
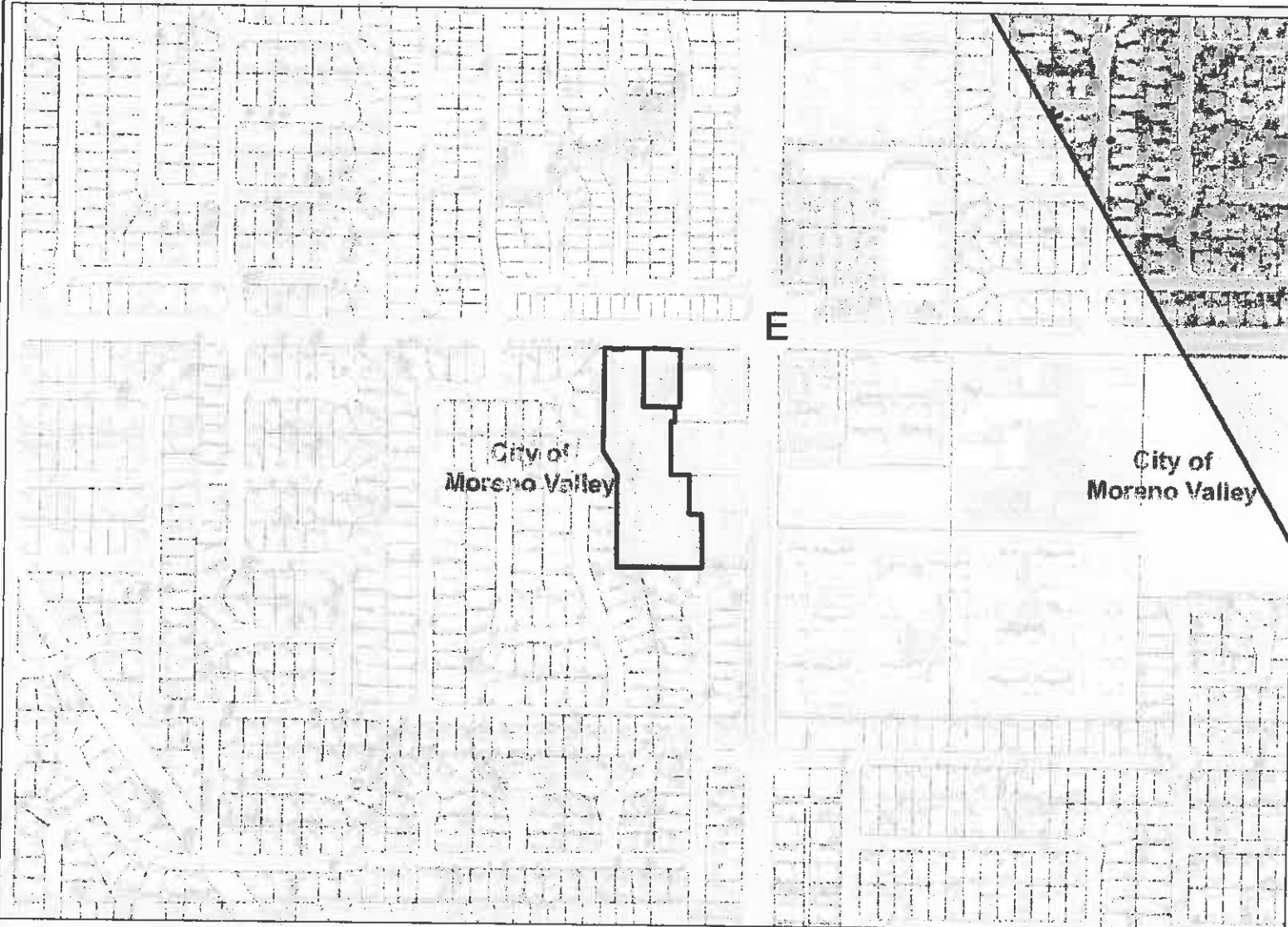
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## Notes

# My Map



## Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



0 646 1,291 Feet



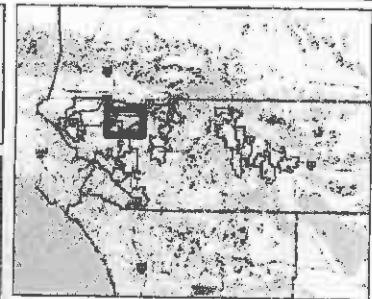
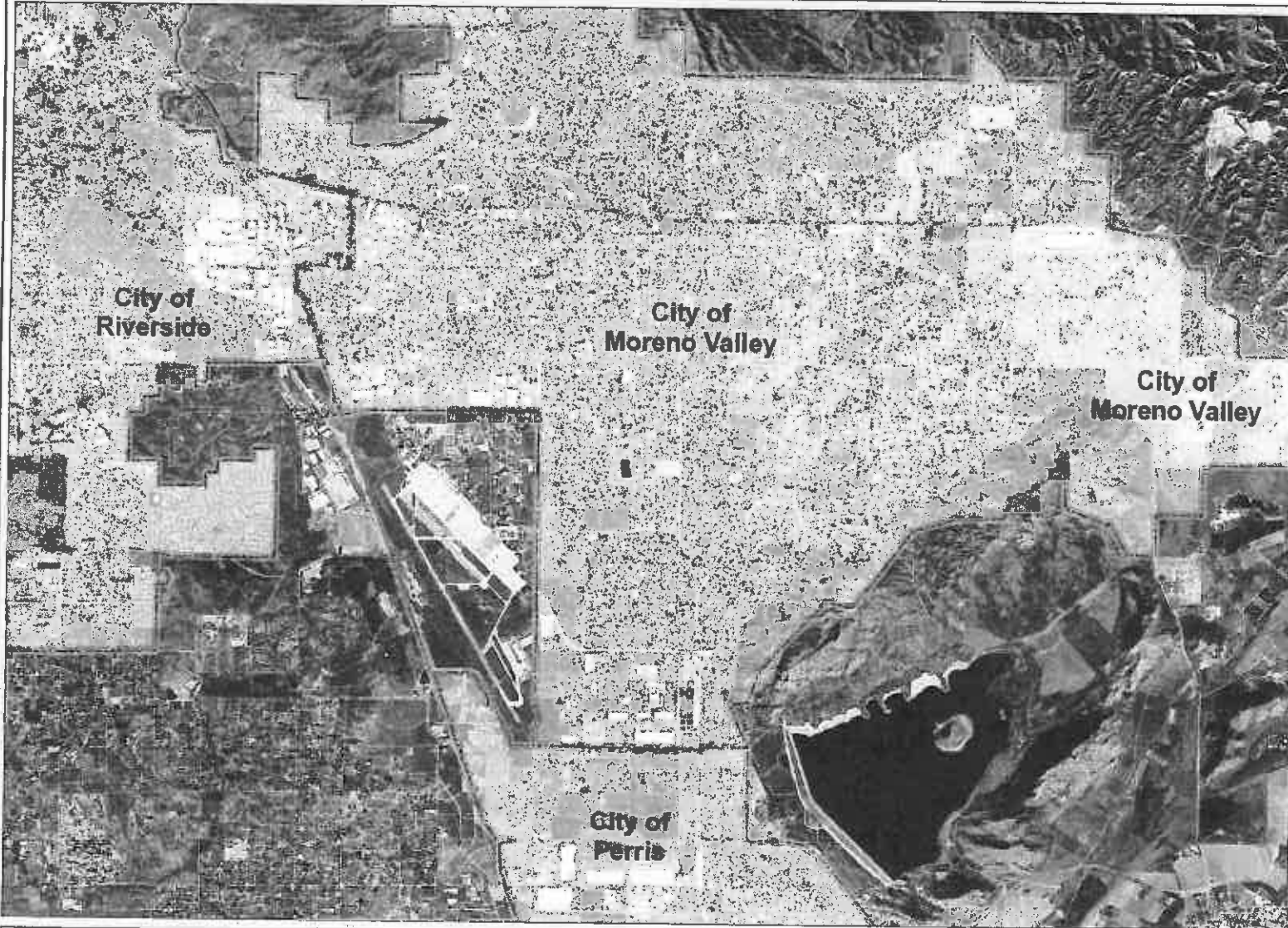
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## Notes

# My Map



## Legend

-  City Boundaries
- Cities



0 10,331 20,662 Feet



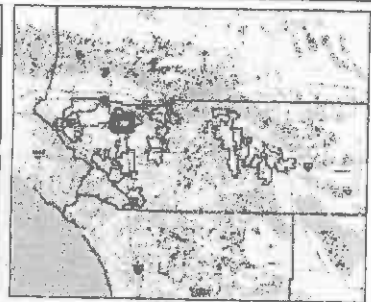
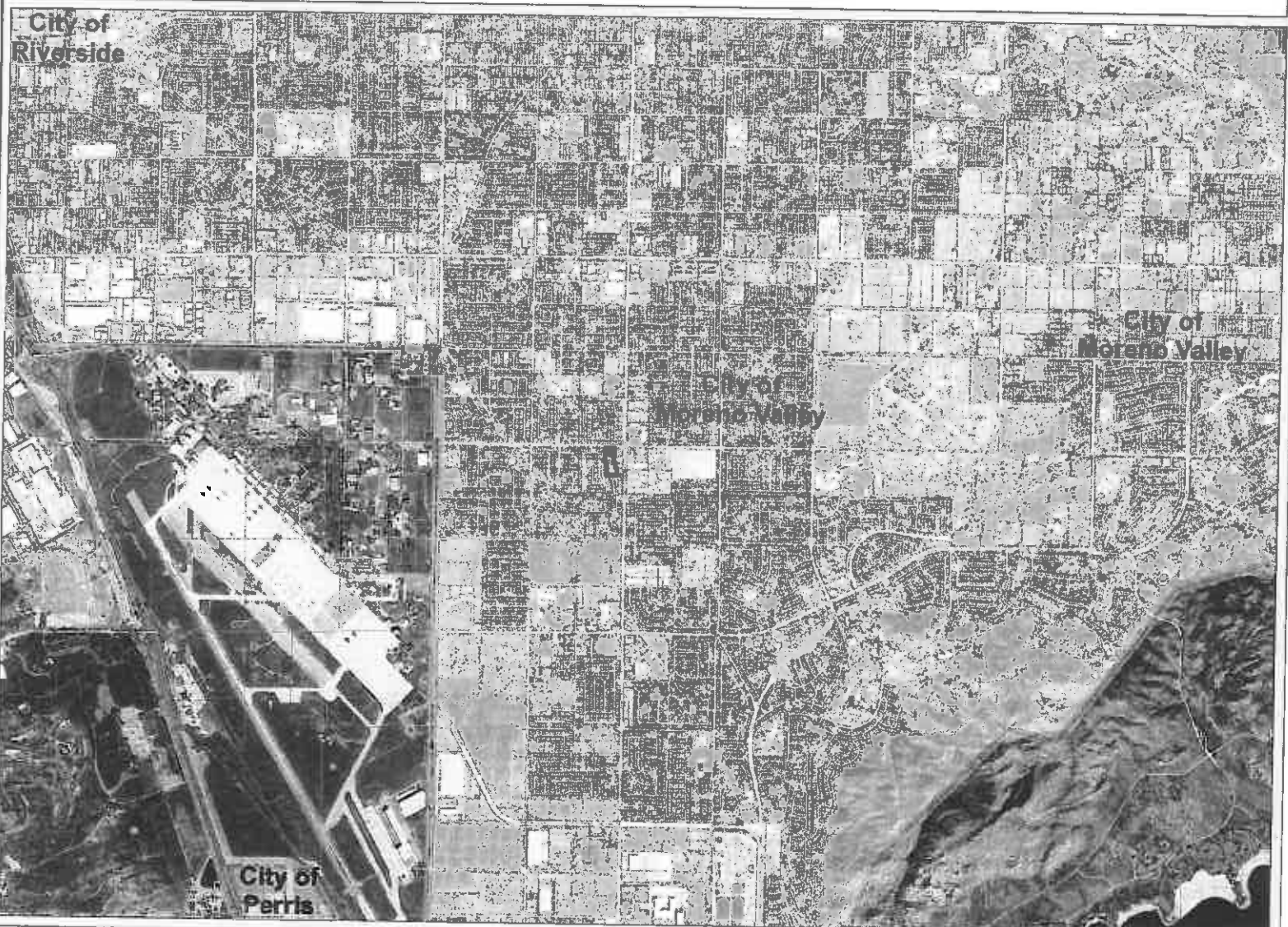
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## Notes

# My Map



**Legend**

-  City Boundaries
-  Cities

**Notes**



0 5,165 10,331 Feet

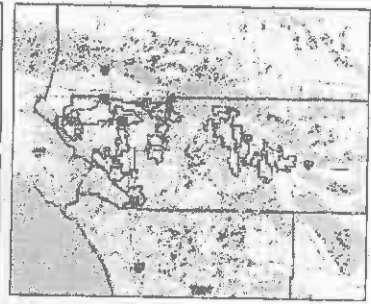


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# My Map



## Legend

-  City Boundaries
- Cities



0 1,291 2,583 Feet



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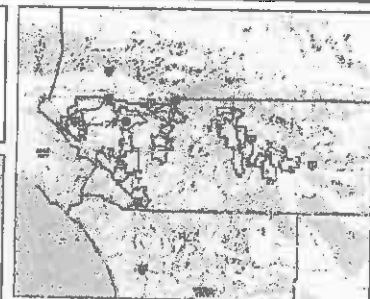
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## Notes



# My Map



## Legend

-  City Boundaries
- Cities



0 646 1,291 Feet



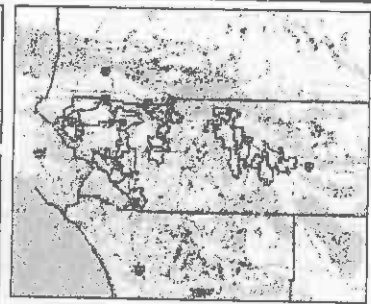
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## Notes

# My Map



## Legend

-  City Boundaries
- Cities



0 323 646 Feet



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## Notes

## CHANGE OF ZONE AND GENERAL PLAN EXPLANATION

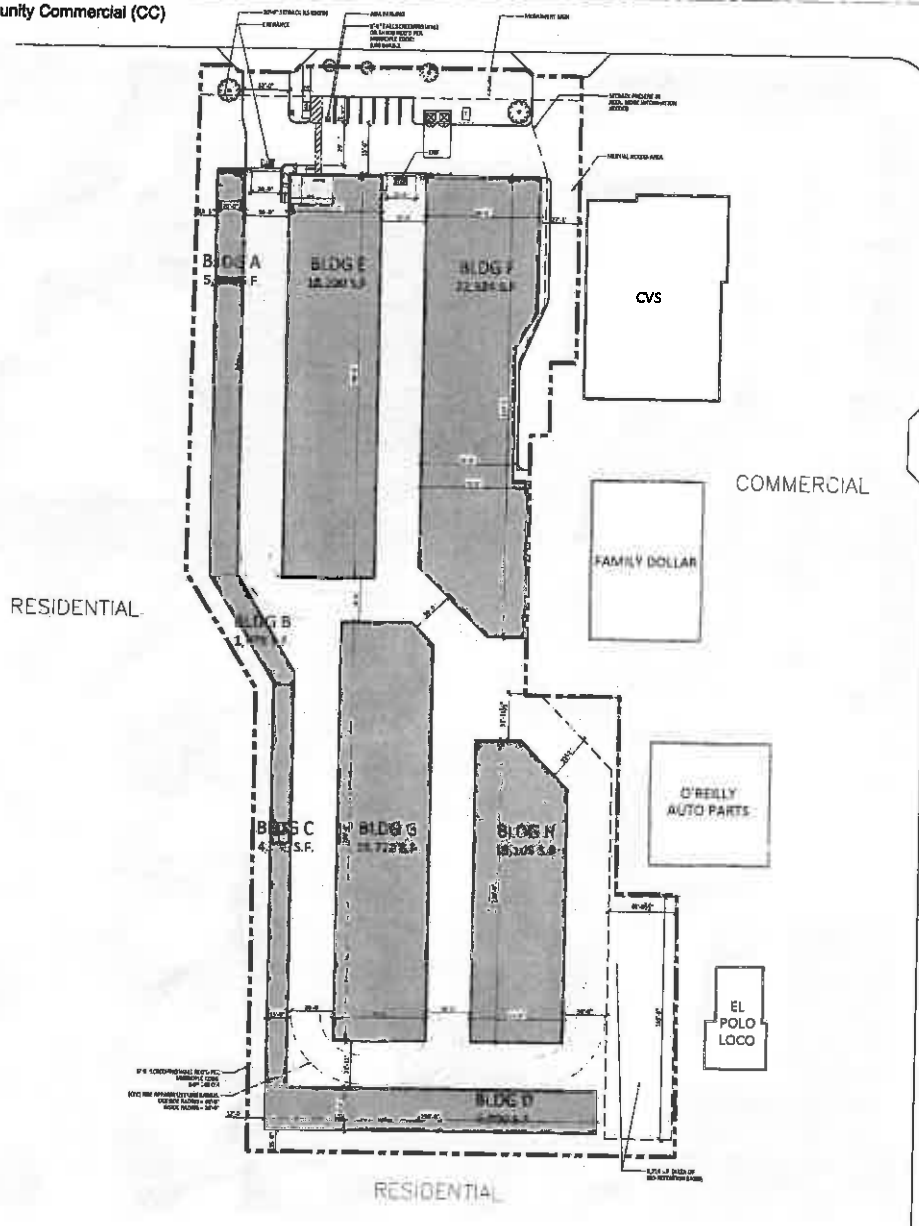
The existing zoning for the subject parcels is Neighborhood Commercial (NC).

The proposed use of the parcels is the development of a mini-warehouse facility. In order to develop the site for such a use, the zoning of the property must be Community Commercial (CC), for which this Change of Zone application is being submitted.

It is also necessary for a Conditional Use Permit (CUP) to be approved for the proposed project to be allowed within the CC zoning designation. With the approval of the CUP for the proposed use within the CC zoning designation, the use will be in concert with the goals, objectives, policies and programs of the General Plan.

Change of Zone from  
Neighborhood Commercial (NC)  
to Community Commercial (CC)

JOHN F. KENNEDY DR.



**PROJECT INFORMATION**

PROJECT ADDRESS: JOHN F KENNEDY DRIVE - MORENO VALLEY CA 92551

PARCEL 1 APN: 485081037  
PARCEL 2 APN: 485081043

SITE AREA: 194,719 S.F. OR 4.47 ACRES

ZONE: NC (NEIGHBORHOOD COMMERCIAL DISTRICT)

GENERAL PLAN: CC (GENERAL COMMERCIAL)

USE: SELF-STORAGE - ALLOWABLE

F.A.R. MAX: 1.00 GENERAL PLAN (2.41 CHAPTER 5)

F.A.R. PROPOSED: 0.62

COVERAGE MAX: N/A

COVERAGE PROPOSED: 63.8%

SETBACKS REQUIRED:  
FRONT: 20'-0"  
SIDE: 0'-0"  
REAR: 0'-0" EQUAL TO BLDG HEIGHT  
EQUAL TO BLDG HEIGHT

SETBACKS PROVIDED:  
FRONT: 61'-0"  
SIDE AT RESIDENTIAL: 12'-0"  
SIDE AT COMMERCIAL: 0'-0"  
REAR: 15'-0"

LS SETBACKS REQ'D:  
FRONT: 20'-0"  
SIDE AT RESIDENTIAL: 12'-0"  
SIDE AT COMMERCIAL: 0'-0"  
SIDE: 12'-0"  
REAR: 15'-0"

1. SETBACK EQUAL TO HEIGHT OF BLDG WHEN ADJACENT TO RESIDENTIAL  
2. IN ALL COMMERCIAL DISTRICTS, REQ'D FRONT SETBACK AREAS SHALL BE LANDSCAPED  
3. REQUIRED REAR AND INTERIOR SIDE BLDG SETBACKS SHALL BE USED ONLY FOR LANDSCAPING, PEDESTRIAN WALKWAYS, DRIVEWAYS, OFF-STREET PARKING OR LOADING, RECREATIONAL ACTIVITIES OR FACILITIES, AND SIMILAR ACTIVITIES

HEIGHT MAXIMUM: SEE SPECIAL REQUIREMENTS

MINIMUM HEIGHT PROPOSED: 10'-8" VARIES

**BUILDING AREA**

PHASE 1 BLDGS:

BLDG A: 5,400 S.F.  
BLDG B: 1,478 S.F.  
BLDG C: 4,178 S.F.  
BLDG D: 6,900 S.F.  
BLDG E: 18,200 S.F.  
BLDG F: 22,324 S.F.  
SUBTOTAL: 56,678 S.F.

PHASE 2 BLDGS:

BLDG G: 18,728 S.F.  
BLDG H: 13,105 S.F.  
SUBTOTAL: 31,833 S.F.

TOTAL PHASE 1 & 2: 90,511 S.F.

**PARKING ANALYSIS**

PARKING REQ'D:

RATIO FOR STORAGE:  
(1,200 STORAGE SPACES)  
541 STORAGE SPACES 6 STALLS

TOTAL REQ'D: 6 STALLS

PROVIDED:  
STANDARD 6 STALLS  
ADA 1 STALLS  
7 STALLS

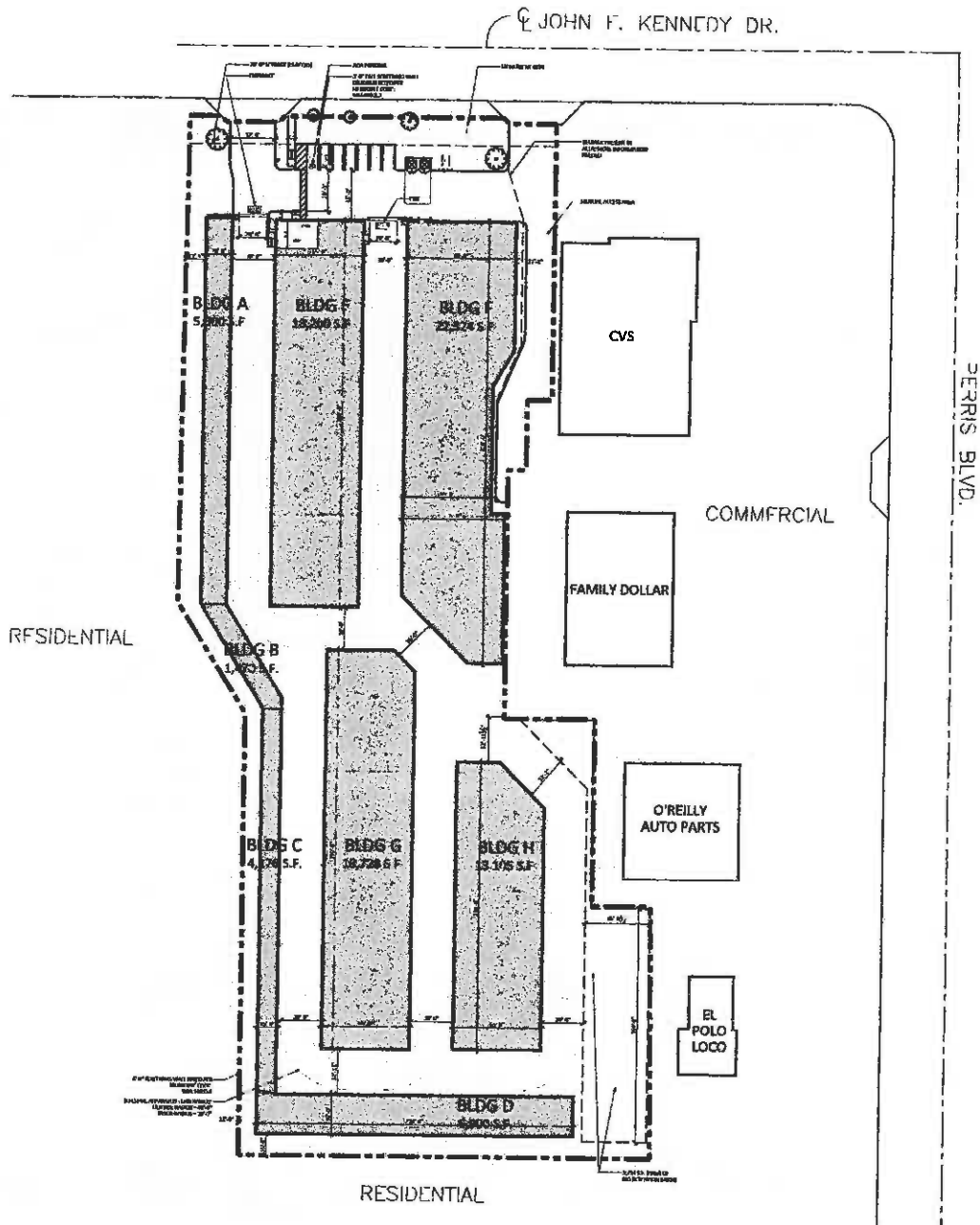


MORENO VALLEY STORAGE

JOHN F KENNEDY DR, MORENO VALLEY CA 92551 09 05 17

KSP PROJECT NO. 20393





**PROJECT INFORMATION**

PROJECT ADDRESS: JOHN F. KENNEDY DRIVE - MORENO VALLEY CA 92551

PARCEL 1 APN: 485081057  
 PARCEL 2 APN: 485081043

SITE AREA: 194,713 S.F. OR 4.47 ACRES

ZONE: NC (NEIGHBORHOOD COMMERCIAL DISTRICT)

GENERAL PLAN: GC (GENERAL COMMERCIAL)

USE: SELF-STORAGE - ALLOWABLE

F.A.R. MAX: 1.00 GENERAL PLAN (2.41 CHAPTER 9)

F.A.R. PROPOSED: 0.52

COVERAGE MAX: N/A

COVERAGE PROPOSED: 62.8%

SETBACKS REQUIRED:  
 FRONT: 20'-0"  
 SIDE: 0'-0"  
 REAR: 0'-0" EQUAL TO BLDG HEIGHT  
 EQUAL TO BLDG HEIGHT

SETBACKS PROVIDED:  
 FRONT: 61'-0"  
 SIDE AT RESIDENTIAL: 12'-0"  
 SIDE AT COMMERCIAL: 0'-0"  
 REAR: 35'-0"

LS SETBACKS REQ'D:  
 FRONT: 20'-0"  
 SIDE AT RESIDENTIAL: 12'-0"  
 SIDE AT COMMERCIAL: 0'-0"  
 SIDE: 12'-0"  
 REAR: 15'-0"

1. SETBACK EQUAL TO HEIGHT OF BLDG WHEN ADJACENT TO RESIDENTIAL
2. IN ALL COMMERCIAL DISTRICTS, REQ'D FRONT SETBACK AREAS SHALL BE LANDSCAPED
3. REQUIRED REAR AND INTERIOR SIDE BLDG SETBACKS SHALL BE USED ONLY FOR LANDSCAPING, PEDESTRIAN WALKWAYS, DRIVEWAYS, OFF-STREET PARKING OR LOADING, RECREATIONAL ACTIVITIES OR FACILITIES, AND SIMILAR ACTIVITIES

HEIGHT MAXIMUM: SEE SPECIAL REQUIREMENTS

MINIMUM HEIGHT PROPOSED: 10'-8" VARIES

**BUILDING AREA**

PHASE 1 BLDGS:	
BLDG A:	5,600 S.F.
BLDG B:	4,728 S.F.
BLDG C:	4,728 S.F.
BLDG D:	6,900 S.F.
BLDG E:	18,280 S.F.
BLDG F:	18,280 S.F.
<b>SUBTOTAL:</b>	<b>58,676 S.F.</b>
PHASE 2 BLDGS:	
BLDG G:	18,278 S.F.
BLDG H:	13,185 S.F.
<b>SUBTOTAL:</b>	<b>31,463 S.F.</b>
<b>TOTAL PHASE 1 &amp; 2:</b>	<b>90,139 S.F.</b>

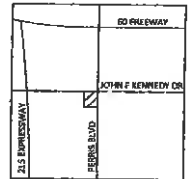
**PARKING ANALYSIS**

PARKING REQ'D:

RATIO FOR STORAGE: (1/100 STORAGE SPACES)  
 541 STORAGE SPACES

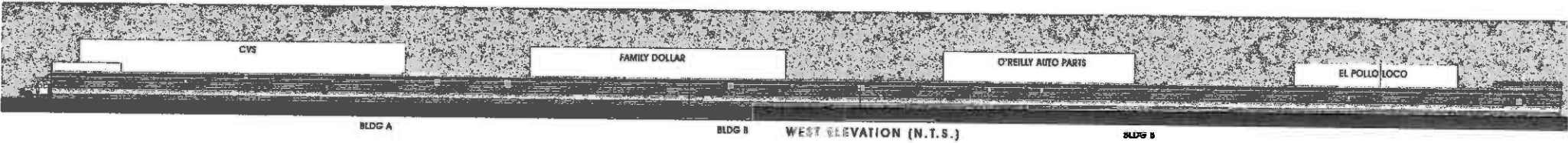
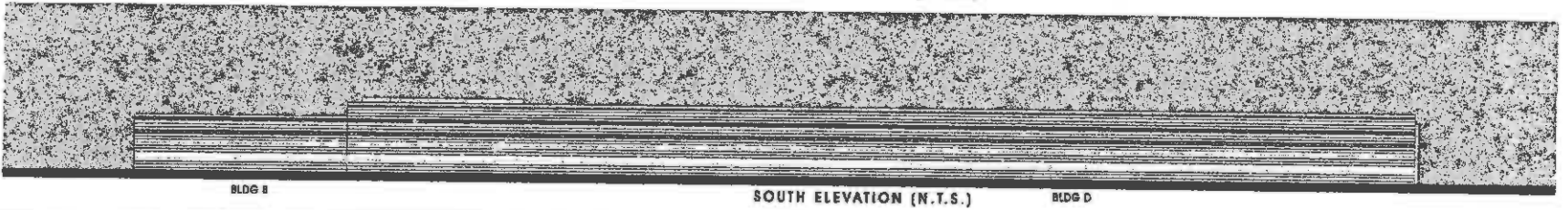
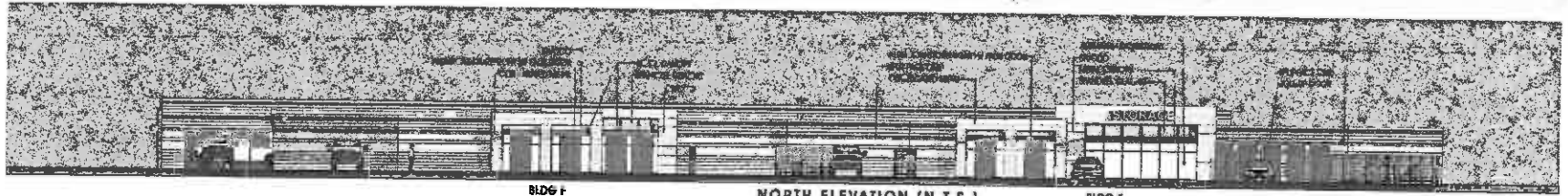
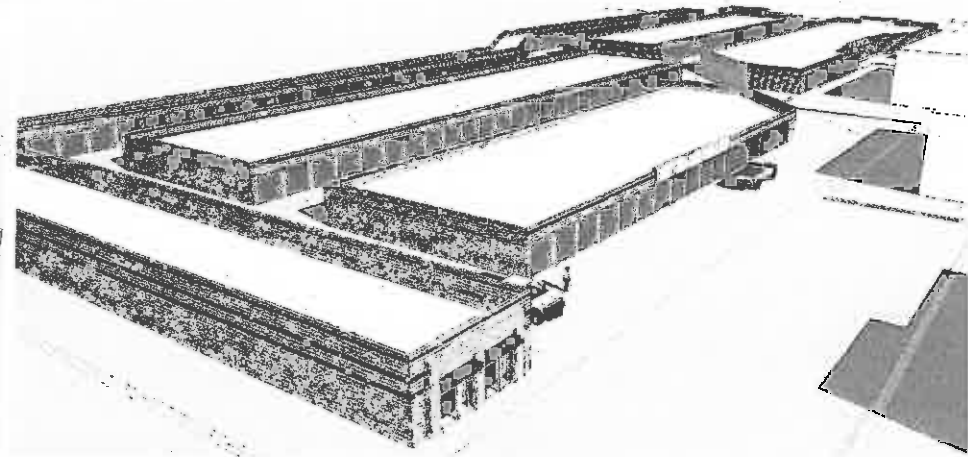
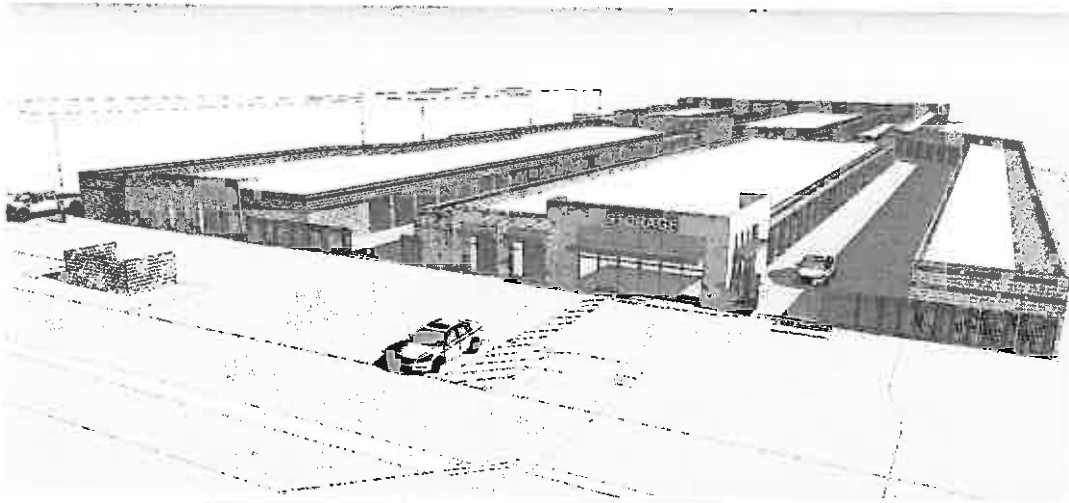
TOTAL REQ'D: 6 STALLS

PROVIDED:  
 STANDARD: 6 STALLS  
 ADA: 3 STALLS  
 TOTAL: 9 STALLS



MORENO VALLEY STORAGE

JOHN F KENNEDY DR, MORENO VALLEY CA 92551 09.03.17



MORENO VALLEY STORAGE

JOHN F KENNEDY DR, MORENO VALLEY CA 92551 09 08 17

KSP PROJECT NO 20183





PAGE BREAK





**AIRPORT LAND USE COMMISSION  
RIVERSIDE COUNTY**



November 9, 2017

**CHAIR**  
Rod Ballance  
Riverside

**VICE CHAIRMAN**  
Steve Manos  
Lake Elsinore

**COMMISSIONERS**

Arthur Butler  
Riverside

John Lyon  
Riverside

Glen Holmes  
Hemet

Russell Betts  
Desert Hot Springs

Steven Stewart  
Palm Springs

**STAFF**  
Director  
Simon A. Housman

John Guerin  
Paul Rull  
Barbara Santos

County Administrative Center  
4680 Lemson St., 14th Floor.  
Riverside, CA 92501  
(951) 955-5132

Mr. Matt Taylor, Project Planner  
City of Riverside Planning Division  
3900 Main Street, 3<sup>rd</sup> Floor  
Riverside CA 92522

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –  
DIRECTOR’S DETERMINATION**

File No.: ZAP1289MA17  
Related File No.: P17-0853 (General Plan Amendment), P17-0781 (Rezone)  
APNs: 211-181-019 thru -022, -024 thru -026

Dear Mr. Taylor:

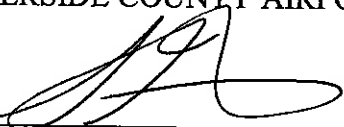
Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to ALUC Resolution No.15-01 (as adopted on August 13, 2015), staff reviewed City of Riverside Case Nos. P17-0781 (Rezone), a proposal to rezone the seven above-referenced parcels totaling 2.04 acres located on the northwest corner of Chicago Avenue and 7<sup>th</sup> Street from Single-Family Residential (R-1-7000) to Multiple-Family Residential (R-3-1500), and P17-0853 (General Plan Amendment), a proposal to amend the land use designation of four of these parcels (211-181-019, -020, -024, -025) from Medium Density Residential to High Density Residential. (There is no associated discretionary development application at this time.)

The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, residential density and non-residential intensity are not restricted.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan (“March ALUCP”).

If you have any questions, please contact Paul Rull, ALUC Urban Regional Planner IV, at (951) 955-6893 or John Guerin, ALUC Principal Planner, at (951) 955-0982.

Sincerely,  
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

  
\_\_\_\_\_  
Simon A. Housman, ALUC Director

Attachments: Notice of Airport in Vicinity

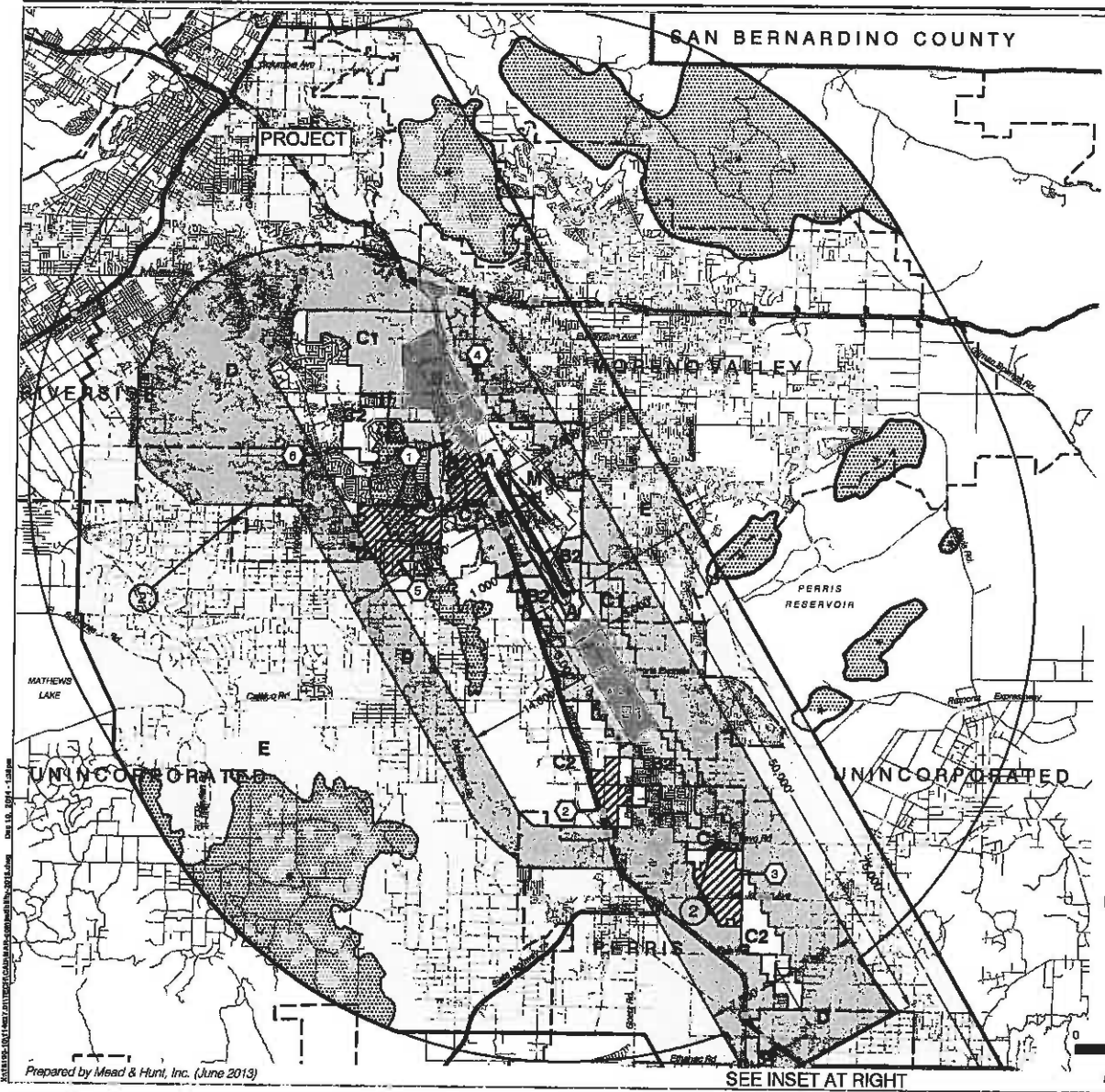
[www.rcaluc.org](http://www.rcaluc.org)

cc: Wakeland Housing & Development Corporation (applicant/property owner)  
Housing Authority of the City of Riverside (property owner)  
Riverside Housing & Development Corporation (property owner)  
Gary Gosliga, Airport Manager, March Inland Port Airport Authority  
Denise Hauser or Daniel Rockholt, March Air Reserve Base  
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1289MA17\ZAP1289MA17.LTR.doc

# NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



**LEGEND**

**Compatibility Zones**

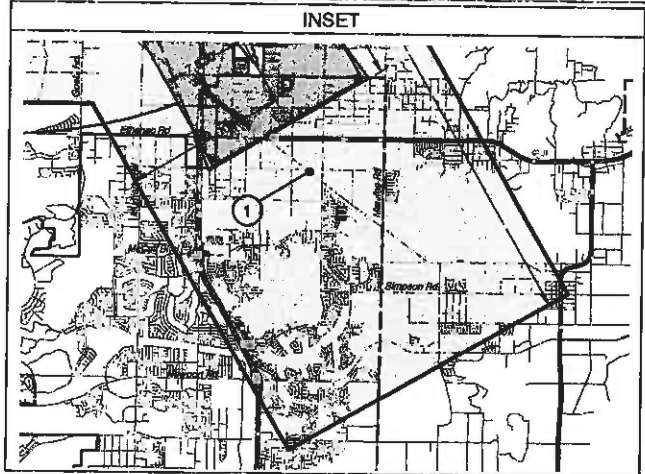
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

**Boundary Lines**

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- ▨ Site-Specific Exceptions (existing local agency commitments to development projects)

- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridga Crest Subdivision



**Riverside County  
Airport Land Use Commission  
March Air Reserve Base / Inland Port Airport  
Land Use Compatibility Plan  
(Adopted November 13, 2014)**

Note:  
All dimensions are measured from runway ends and centerlines.



Base map source: County of Riverside 2013

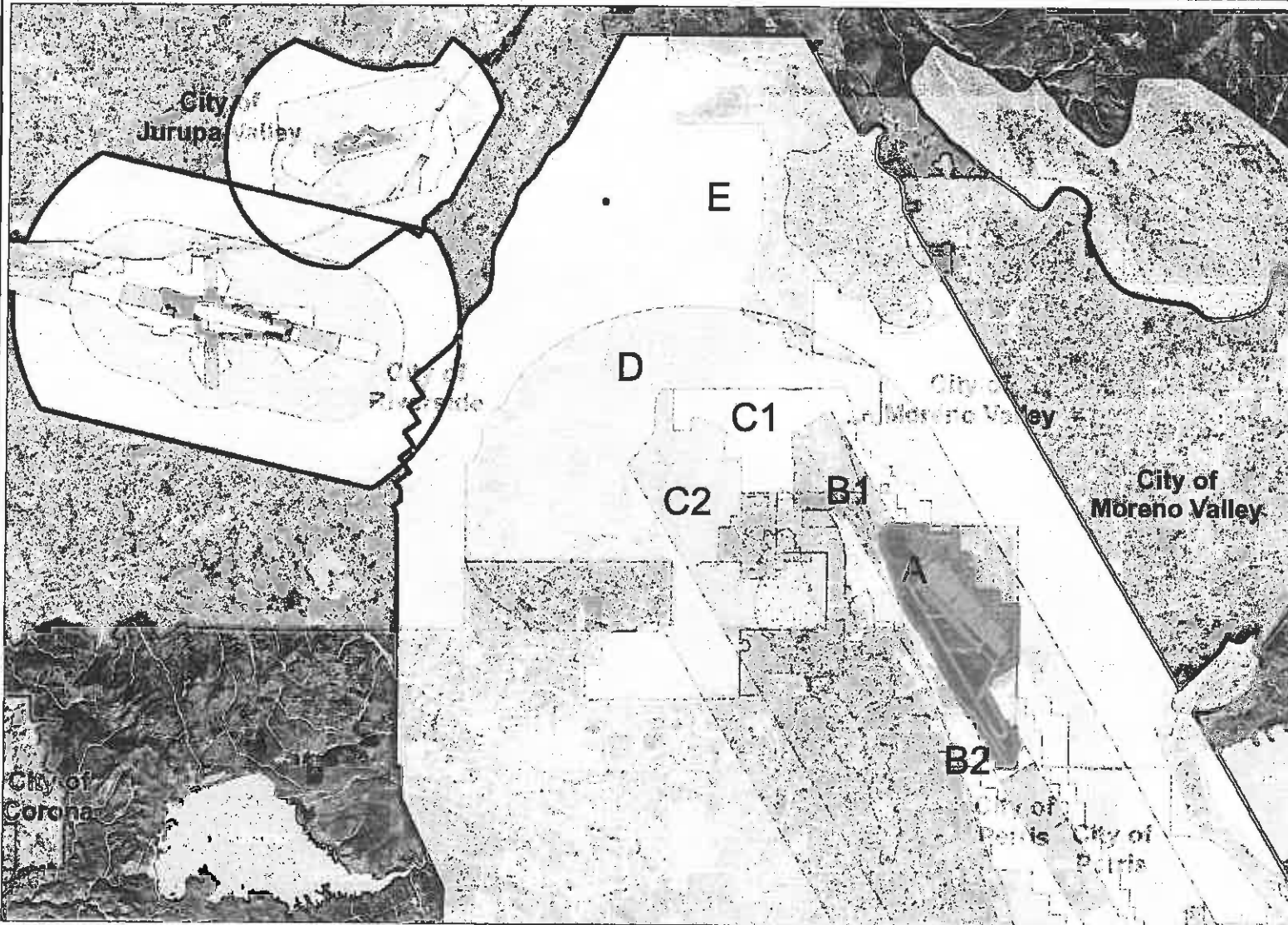
Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map MA-1

**Compatibility Map**  
March Air Reserve Base / Inland Port Airport

# My Map



## Legend

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



0 14,232 28,465 Feet



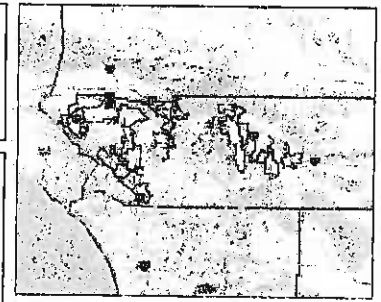
**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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## Notes

# My Map



**Legend**

- Airports
- AIA
- Airport Compatibility**
- OTHER ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
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- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6
- C2-HIGHT



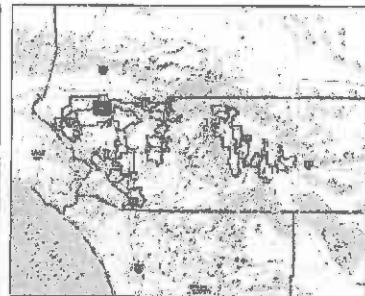
**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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**Notes**

# My Map



## Legend

-  City Boundaries
- Cities
- roadsanno
- highways
  -  HWY
  -  INTERCHANGE
  -  INTERSTATE
  -  OFFRAMP
  -  ONRAMP
  -  USHWY
- roads
  -  Major Roads
  -  Arterial
  -  Collector
  -  Residential
-  counties
-  cities
- hydrographylines
- waterbodies
  -  Lakes
  -  Rivers



0 3,336 6,671 Feet



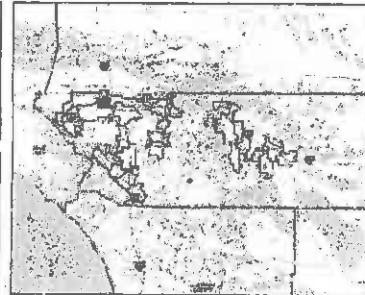
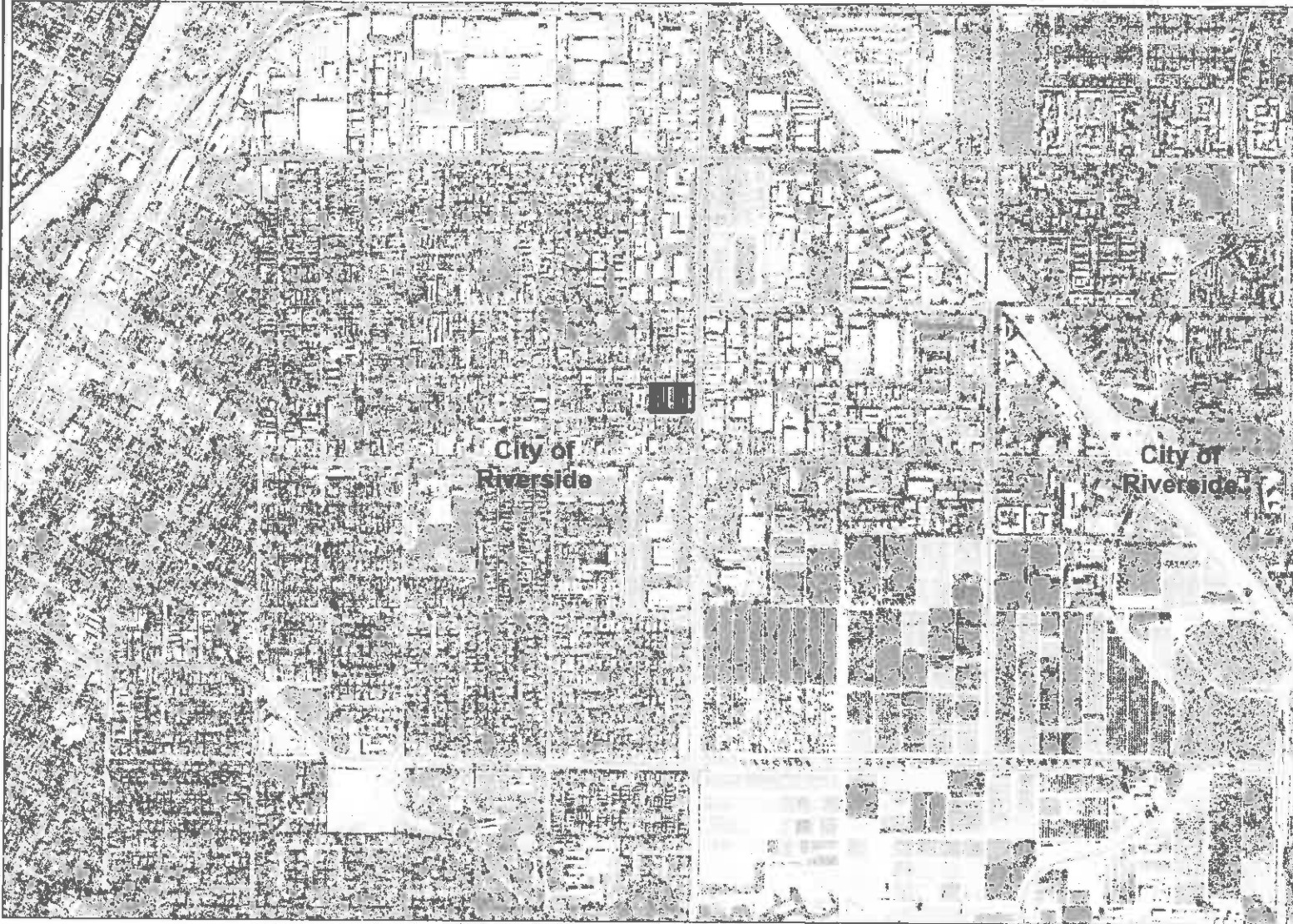
**\*IMPORTANT\*** Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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## Notes

# My Map



## Legend

- City Boundaries
- Cities
- roadsanno
- highways
  - HWY
  - INTERCHANGE
  - INTERSTATE
  - OFFRAMP
  - ONRAMP
  - USHWY
- counties
- cities
- hydrographylines
- waterbodies
  - Lakes
  - Rivers



0 1,668 3,336 Feet



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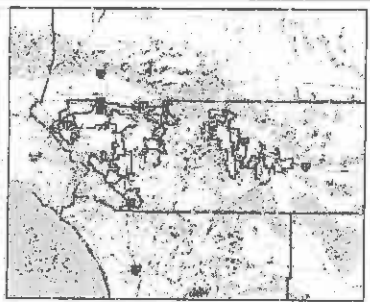
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## Notes



# My Map



### Legend

- City Boundaries
- Cities
- roadsanno
- highways
  - HWY
  - INTERCHANGE
  - INTERSTATE
  - OFFRAMP
  - ONRAMP
  - USHWY
- counties
- cities
- hydrographylines
- waterbodies
  - Lakes
  - Rivers



0 417 834 Feet



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### Notes

# My Map



## Legend

- City Boundaries
- Cities
- roadsanno
- highways
- HWY
- INTERCHANGE
- INTERSTATE
- OFFRAMP
- ONRAMP
- USHWY
- counties
- cities
- hydrography
- lines
- waterbodies
- Lakes
- Rivers



0 104 208 Feet



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## Notes

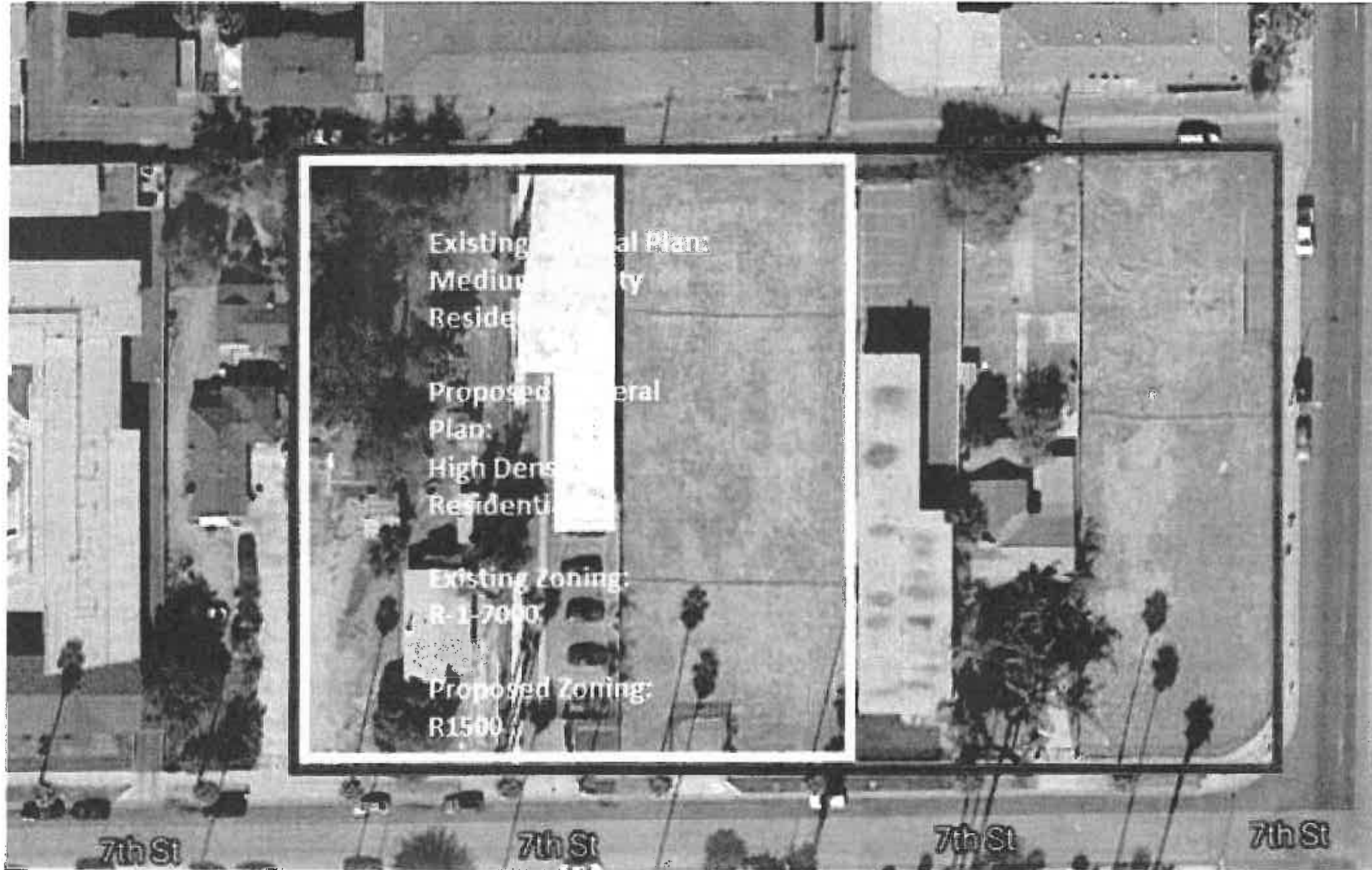
Affected Parcel #s (subject site)	Street Address	Property Owner	Parcel Size	Existing GP	Existing Zoning	Proposed GP	Proposed Zoning
211-181-026	1705 7 <sup>th</sup> Street, Riverside	Housing Authority of the City of Riverside	.36	HDR (29 du/ace)	R-1-7000	No Change	R-3-1500
211-181-022	1719 7 <sup>th</sup> Street, Riverside	Wakeland Entrada, LP	.24	HDR	R-1-7000	No Change	R-3-1500
211-181-021	1725 7 <sup>th</sup> Street, Riverside	Housing Authority of the City of Riverside	.24	HDR	R-1-7000	No Change	R-3-1500
211-181-020	1733 7 <sup>th</sup> Street, Riverside	Housing Authority of the City of Riverside	.48	MDR (8 du/ac)	R-1-7000	HDR (29 du/ac)	R-3-1500
211-181-019	1747 7 <sup>th</sup> Street, Riverside	Wakeland Entrada, LP	.24	MDR	R-1-7000	HDR (29 du/ac)	R-3-1500
211-181-024	1753 7 <sup>th</sup> Street, Riverside	Riverside Housing Development Corporation	.24	MDR	R-1-7000	HDR (29 du/ac)	R-3-1500
211-181-025	1761 7 <sup>th</sup> Street, Riverside	Riverside Housing Development Corporation	.24	MDR	R-1-7000	HDR (29 du/ac)	R-3-1500
		<b>Total Project Site:</b>	2.04 Acres				

The project site (described above) total 2.04 acres, is generally rectangular in shape and located at the North West corner of Chicago Avenue and 7<sup>th</sup> Street, in the Eastside Neighborhood.

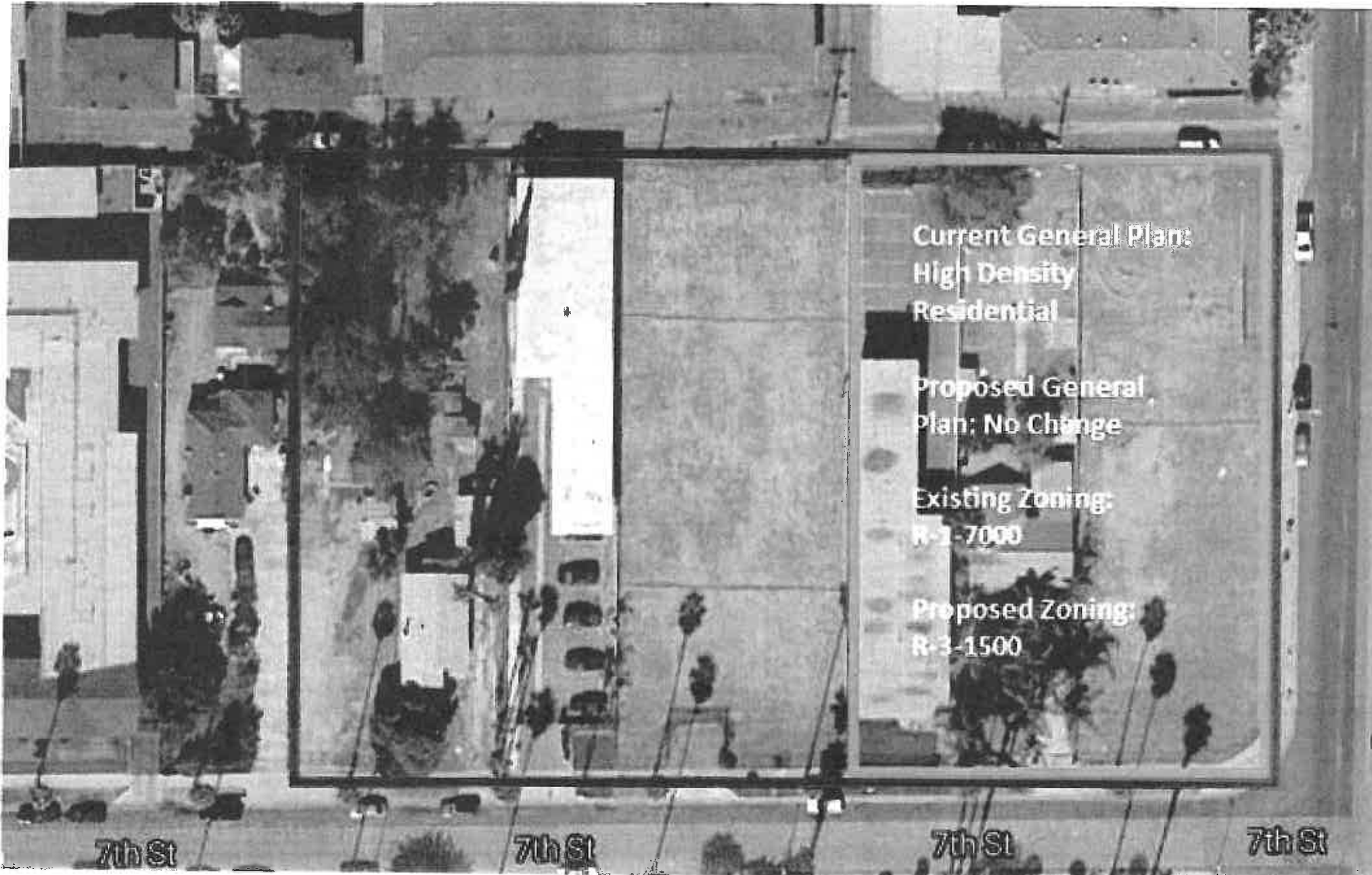
TOTAL PROJECT SITE



Yellow - GP & Zoning



Green = Zonings



**AIRPORT LAND USE COMMISSION  
MINUTE ORDER NOVEMBER 9, 2017  
RIVERSIDE MEETING**

A regular scheduled meeting of the Airport Land Use Commission was held on November 9, 2017 at the Riverside County Administrative Center, Board Chambers.

**COMMISSIONERS PRESENT:** Rod Ballance, Chairman  
Linda, Krupa, alternate for Steve Manos  
Russell Betts  
Arthur Butler  
Glen Holmes  
John Lyon  
Steven Stewart

**COMMISSIONERS ABSENT:** Steve Manos

**STAFF PRESENT:** Simon Housman, ALUC Director  
John Guerin, Principal Planner  
Paul Rull, Urban Regional Planner IV  
Barbara Santos, ALUC Commission Secretary  
Raymond Mistica, ALUC Counsel

**OTHERS PRESENT:** Sam Akbarpou, Other Interested Person  
Mel Mercado, California Baptist University

**AIRPORT LAND USE COMMISSION  
MINUTE ORDER NOVEMBER 9, 2017  
RIVERSIDE MEETING**

- I. **AGENDA ITEM 3.1:** ZAP1077FV17 – Newland Homes, LLC (Representative: EPD Solutions, Inc. Rafik Albert) – County of Riverside Planning Case Nos. CZ7937 (Change of Zone), TR37294 (Tentative Tract Map), PP26249 (Plot Plan). Tentative Tract Map No. 37294 is a proposal to divide 12.5 gross acres located northerly of Los Alamos Road, southerly of De Caron Street, easterly of Suzi Reid Way and westerly of Briggs Road into 48 single family residential lots. The applicant also proposes to change the zoning of the property from Rural Residential (R-R) to Planned Residential (R-4). Plot Plan No. 26249 depicts building elevations and floor plans for the 48 proposed residences. (Airport Compatibility Zone D of the French Valley Airport Influence Area). ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at [prull@rivco.org](mailto:prull@rivco.org)

II. **MAJOR ISSUES**  
None

- III. **STAFF RECOMMENDATION**  
Staff recommends that the Commission find the proposed Change of Zone CONSISTENT with the 2007 French Valley Airport Land Use Compatibility Plan as amended in 2011, and find the proposed Tentative Tract Map CONSISTENT, subject to the conditions included herein.

- IV. **PROJECT DESCRIPTION**  
Tentative Tract Map No. 37294 is a proposal to divide 12.5 gross acres into 48 single family residential lots. The applicant also proposes to change the zoning of the property from Rural Residential (R-R) to Planned Residential (R-4). Plot Plan No. 26249 depicts building elevations and floor plans for the 48 proposed residences.

**CONDITIONS:**

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky and shall comply with the Riverside County Ordinance No. 655. Outdoor lighting shall be downward facing.
2. The review of this Tentative Tract is based on the proposed uses and activities noted in the project description. The following uses/activities are not included in the proposed project and shall be prohibited at this site, in accordance with Note A on Table 4 of the Southwest Area Plan.
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area.



**AIRPORT LAND USE COMMISSION  
MINUTE ORDER NOVEMBER 9, 2017  
RIVERSIDE MEETING**

- (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
- 3. The attached notice shall be provided to all potential purchasers of the lots and to the tenants of the homes thereon, and shall be recorded as a deed notice.
- 4. The following uses/activities are specifically prohibited at this location: trash transfer stations that are open on one or more sides; recycling centers containing putrescible wastes; construction and demolition debris facilities; wastewater management facilities; incinerators; noise-sensitive outdoor nonresidential uses; and hazards to flight.
- 5. Any proposed detention basin or facilities shall be designed so as to provide for a detention period for the design storm that does not exceed 48 hours and to remain totally dry between rainfalls. Vegetation in and around such facilities that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature.
- 6. The ALUC eligible open area provided in Lot E (minimum of 1.26 acres) shall be kept obstacle and obstruction free per ALUC open area definition.

**V. MEETING SUMMARY**

The following staff presented the subject proposal:

ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at [prull@rivco.org](mailto:prull@rivco.org)

No one spoke in favor, neutral or opposition to the project.

**VI. ALUC COMMISSION ACTION**

The ALUC Commission by a unanimous vote of 7-0 found the project **CONSISTENT**.

**VII. CD**

The entire discussion of this agenda item can be found on CD and referenced by the meeting time listed below. For a copy of the CD, please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at [basantos@rivco.org](mailto:basantos@rivco.org).

ITEM 3.1: TIME: 9:02 A.M.

**AIRPORT LAND USE COMMISSION  
MINUTE ORDER NOVEMBER 9, 2017  
RIVERSIDE MEETING**

I. **AGENDA ITEM 3.2:** ZAP1032FL17 – Amy Han (Representative: Sake Engineering, Sam Akbarpour) – City of Jurupa Valley Planning Case No. MA16119 (Tentative Tract Map 37109). The applicant proposes to subdivide a 1.75-acre parcel (Assessor's Parcel No. 182-303-005) located westerly of the northerly end of Renee Avenue, southerly of Mission Boulevard, northerly of Janet Street, and easterly of Hare Avenue into 7 single family residential lots. The site includes a residence with an address of 4077 Riverview Drive. (Airport Compatibility Zones D and E of the Flabob Airport Influence Area). ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at [prull@rivco.org](mailto:prull@rivco.org)

II. **MAJOR ISSUES**

The proposed project results in a density of 4.0 dwelling units per gross acre, which is inconsistent with the Compatibility Zone D minimum density criteria of less than 0.2 dwelling units per acre or greater than 5.0 dwelling units per acre. However, density in Zone D is calculated by net acreage, and if the total number of dwelling units is divided by only the acreage in Zone D, the resulting density would be 5.1 dwelling units per acre. Also, if the total number of dwelling units is divided by the total acreage minus the cul-de-sac street, the resulting density would be 5.0 dwelling units per acre.

III. **STAFF RECOMMENDATION**

Staff recommends that the Commission find the Tentative Tract Map INCONSISTENT, based on the density being within the prohibited intermediate density range.

IV. **PROJECT DESCRIPTION**

The applicant proposes to subdivide a 1.75-acre parcel (Assessor's Parcel No. 182-303-005) into 7 single family residential lots.

**CONDITIONS:** (New conditions, as added pursuant to the FAA letter subsequent to hearing, shown in **bold type**).

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, artificial marshes, wastewater management facilities, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash

**AIRPORT LAND USE COMMISSION  
MINUTE ORDER NOVEMBER 9, 2017  
RIVERSIDE MEETING**

disposal, and incinerators.)

- (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Highly noise-sensitive outdoor nonresidential uses.
3. The attached notice shall be provided to all potential purchasers of the proposed lots, and shall be recorded as a deed notice.
  4. Any ground-level or aboveground water retention or detention basin or facilities shall be designed so as to provide for a detention period for the design storm that does not exceed 48 hours and to remain totally dry between rainfalls. Vegetation in and around such facilities that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature.
  5. Prior to the issuance of building permits for any buildings or structures exceeding a top roof elevation of 834 feet above mean sea level, the applicant shall have filed Form 7460-1 online (see [www.oceaaa.faa.gov](http://www.oceaaa.faa.gov)) with the Federal Aviation Administration Obstruction Evaluation Service (FAA OES) and shall have presented to the City's Department of Building a "Determination of No Hazard to Air Navigation" letter issued by the FAA OES. The applicant shall comply with all requirements of said letter.

*[Note: The above Condition No. 5 is effectively superseded by Conditions Nos. 6 through 10, below.]*

**The following conditions have been added subsequent to the ALUC hearing pursuant to the terms of the FAA Obstruction Evaluation Service letter issued on November 13, 2017 for Aeronautical Study No. 2017-AWP-10942-OE.**

6. **The Federal Aviation Administration has conducted an aeronautical study of the proposed project (Aeronautical Study No. 2017-AWP-10942-OE) and has determined that neither marking nor lighting of the structure(s) is necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 L Change 1 and shall be maintained in accordance therewith for the life of the project.**
7. **The proposed structure(s) shall not exceed a height of 30 feet above ground level and a maximum elevation at top point (including all roof-mounted equipment, if any) of 842 feet above mean sea level.**
8. **The maximum height and top point elevation specified above shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in structure height or elevation shall not require further review by the Airport Land Use Commission.**

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9. **Temporary construction equipment used during actual construction of the structure(s) shall not exceed 30 feet in height and a maximum elevation of 842 feet above mean sea level, unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.**
10. **Within five (5) days after construction reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oecaaa.faa.gov> for instructions.) This requirement is also applicable in the event the project is abandoned or a decision is made not to construct the structure(s).**

**V. MEETING SUMMARY**

The following staff presented the subject proposal:

ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at [prull@rivco.org](mailto:prull@rivco.org)

The following spoke in favor of the project:

Sam Akbarpou, Other Interested Person, Corona, CA 92879

No one spoke in neutral or opposition to the project.

**VI. ALUC COMMISSION ACTION**

The ALUC Commission by a vote of 6-0 found the project **CONDITIONALLY CONSISTENT** subject to FAA conditions. Recuse: Commissioner John Lyon

**VII. CD**

The entire discussion of this agenda item can be found on CD and referenced by the meeting time listed below. For a copy of the CD, please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at [basantos@rivco.org](mailto:basantos@rivco.org).

ITEM 3.2: TIME: 9:07 A.M.

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I. **AGENDA ITEM 3.3:** ZAP1090RI17 – California Baptist University - (Representatives: Steve Smith, Mark Howe, and David Leonard) – City Planning Case Nos. P15-0989 (General Plan Amendment), P15-0987 (Rezone), and P17-0543 (Specific Plan Amendment). California Baptist University (CBU) proposes to comprehensively revise its Specific Plan for the continued development of its campus. The core area of the campus includes all of the land within the area bounded by Magnolia Avenue on the north, Adams Street on the east, Monroe Street on the west, and Diana Road/State Highway Route 91 on the south, except for the parcel (Assessor's Parcel Number 231-080-009) at the northwest corner of Adams and Diana/Highway 91 developed with a service station and food mart and a lot at 3476 Wilma Court. The campus also includes discrete parcels fronting on the north side of Magnolia Avenue and the east side of Adams Street. The Specific Plan Amendment would incorporate 10.93 acres (two parcels) fronting on the west side of Monroe Street (Assessor's Parcel Numbers 233-120-010 and 233-110-045) owned by CBU into the Specific Plan (deleting them from the Magnolia Avenue Specific Plan). P15-0988 would amend the General Plan designation of these two parcels from Public Facilities/Institutional (PF) to CBU Specific Plan (CBUSP), while P15-0987 would rezone the two parcels from R-1-7,000 (Residential, 7,000 square foot minimum lot size) to CBU SP-2. P15-0987 would also rezone the core campus from CBUSP-A, CBUSR-MU/A, CBUSP-MU/R, CBUSP-MU/U and CBUSP-OS to CBU SP-1 and would rezone the parcels fronting on the north side of Magnolia Avenue and the east side of Adams Street from CBUSP-MU/A and CBUSP-MU/R to CBU SP-2. (Compatibility Zones D and E of the Riverside Municipal Airport Influence Area). ALUC Staff Planner: John Guerin at (951) 955-0982, or e-mail at [jguerin@rivco.org](mailto:jguerin@rivco.org)

II. **MAJOR ISSUES**

Compliance with Airport Compatibility Zone D non-residential intensity limitations: the Specific Plan, as proposed, would allow reuse of existing buildings in Zone D without the ability for the City to assure that the resulting intensity would comply with ALUCP compatibility policies in effect at the time of the proposed modification.

III. **STAFF RECOMMENDATION**

Staff recommends that the proposed General Plan Amendment (adding two parcels to the area covered by the Specific Plan) be found CONSISTENT, but that the Specific Plan Amendment and Rezoning project be found INCONSISTENT with the 2005 Riverside Municipal Airport Land Use Compatibility Plan. However, in the event that the applicant amends the Specific Plan text and tables so as to demonstrate a commitment that any new structures or changes in use on lands within Airport Compatibility Zone D will be consistent with intensity criteria for this zone, staff would recommend that the Specific Plan Amendment and Rezoning be found CONSISTENT.

**STAFF RECOMMENDED AT HEARING**  
CONSISTENT (SPA, Rezoning, GPA)

IV. **PROJECT DESCRIPTION**

California Baptist University (CBU) proposes to comprehensively revise its Specific Plan for the continued development of its campus. In addition to the text and table revisions within the Specific Plan document, the Specific Plan Amendment would incorporate 10.93 acres (two parcels) fronting on the west side of Monroe Street into the Specific Plan, deleting them from the Magnolia Avenue Specific Plan. P15-0988 would amend the General Plan designation of these two parcels from Public Facilities/Institutional (PF) to CBU Specific Plan (CBUSP), while P15-0987 would rezone the two parcels from R-1-7,000 (Residential, 7,000 square foot minimum lot size) to CBU SP-2. P15-0987 would also rezone the core campus area bounded by Magnolia Avenue on the north, Adams Street on the east, Monroe Street on the west, and Diana Road/State Highway Route 91 on the south (except for the parcel at the northwest corner of Adams and Diana/Highway 91 developed with a

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service station and food mart and a single-family residential lot at 3476 Wilma Court) from CBUSP-A, CBUSP-MU/A, CBUSP-MU/R, CBUSP-MU/U and CBUSP-OS to CBU SP-1 and would rezone the parcels fronting on the north side of Magnolia Avenue and on the east side of Adams Street from CBUSP-MU/A and CBUSP-MU/R to CBU SP-2.

The campus includes over 815,000 square feet of existing academic, athletic, and recreation buildings (see Table 2-4 of the proposed Specific Plan) and ten residential facilities providing for a capacity of 2,976 beds. CBU officials envision an additional 400,000 square feet of building area by the year 2025, bringing the total to 1,215,114 square feet. Two parking structures are also planned, providing for a combined capacity of 2,767 parking spaces. The intent is to serve an enrollment of 12,000 (including 2,921 online) students by that year.

**CONDITIONS:**

1. Any outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Lights must be downward facing.
2. The following uses shall be prohibited:
  - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
  - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
  - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area, including landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, and incinerators
  - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
  - (e) Within the Zone D portion of the site: Children's schools, hospitals, nursing homes, and outdoor noise-sensitive uses including, but not limited to, amphitheaters.
3. The attached notice shall be provided to all potential purchasers and tenants, and shall be recorded as a deed notice.
4. Any ground-level or aboveground water retention or detention basin or facilities shall be designed so as to provide for a detention period for the design storm that does not exceed 48 hours and to remain totally dry between rainfalls. Vegetation in and around such facilities that would provide food or cover for bird species that would be incompatible with airport operations shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large

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expenses of contiguous canopy, when mature.

In the event that the requirements of this condition cannot be met, CBU shall work with the City Airport Department and a qualified bird strike/wildlife hazard management consultant to prepare a Wildlife Hazard Management Plan that is acceptable to both the airport operator and the United States Department of Agriculture Wildlife Services agency.

5. Prior to issuance of building permits for any new structure or remodeling that would increase the height of any existing structure, CBU shall submit documentation verifying that the structure's elevation above mean sea level (at top point, including all roof-mounted equipment and lighting, if applicable): (1) will not exceed the elevation of Runway 16-32 at its southerly terminus (747.5 feet above mean sea level) by more than one foot for every 100 feet of distance from the structure to that runway; and, (2) will not exceed the elevation of Runway 9-27 at its easterly terminus (815 feet above mean sea level) by more than one foot for every 100 feet of distance from the structure to that runway. If both of these requirements cannot be met for a given structure, the applicant shall file Form 7460-1 with the Federal Aviation Administration, and no building permit shall be issued until a "Determination of No Hazard to Air Navigation" is received from the Federal Aviation Administration and filed with the City of Riverside Planning Department, the City of Riverside Building and Safety Department, and the Riverside County Airport Land Use Commission.

**V. MEETING SUMMARY**

The following staff presented the subject proposal:

ALUC Staff Planner: Paul Rull at (951) 955-6893, or e-mail at [prull@rivco.org](mailto:prull@rivco.org)

The following spoke in favor of the project:

Mel Mercado, California Baptist University, 8432 Magnolia Ave., Riverside, CA

No one spoke in neutral or opposition to the project.

**VI. ALUC COMMISSION ACTION**

The ALUC Commission by a unanimous vote of 7-0 found the project **CONSISTENT** (SPA, Rezoning, GPA).

**VII. CD**

The entire discussion of this agenda item can be found on CD and referenced by the meeting time listed below. For a copy of the CD, please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at [basantos@rivco.org](mailto:basantos@rivco.org).

ITEM 3.3: TIME: 9:36 A.M.

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**I. 4.0 ADMINISTRATIVE ITEMS**

**4.1 Director's Approvals – Information only**

**4.2 Presentation: Countywide Amendment**

Chairman Ballance advised that he concurs with formation of a subcommittee and appointed Commissioners Lyon and Manos as additional members. Commissioner Holmes inquired as to the potential for establishing a fee on development in AIAs that could be used toward purchase of lands that would remain open areas available for emergency landing, rather than the less-usable half-acres gained through open area percentage requirements. Director Housman indicated that the subcommittee could consider this suggestion. ALUC Counsel Ray Mistica advised that if Vice-Chairman Manos declines, the matter of subcommittee membership can be revisited at next month's ALUC meeting.

**4.3 2018 ALUC Schedule**

Director Housman advised that this would be an appropriate time to determine any "dark" month for 2018. He is recommending February as the "dark" month. Commissioner Holmes asked whether a special meeting could be set, if necessary. Director Housman agreed that the Commission will continue to be sensitive to the needs of the community. Chairman Ballance noted that December 14 would be a full agenda. He asked if a motion was needed. ALUC Counsel Mistica responded that a motion was not necessary, just a need to seek consensus and determine if there are any objections. There were none, so the Commission will anticipate not meeting in February.

**4.4 ZAP1291MA17 Request for Delegation of Authority**

Director Housman noted that the proposed project is a parking lot in the C2 zone. He is requesting special delegation for efficiency. Chairman Ballance indicated support for delegation in this particular circumstance. Commissioner Holmes moved for delegation, Commissioner Lyon seconded, and the delegation was unanimously supported.

**4.5 New ALUC Logo**

Possibilities for new ALUC logos were presented to the Commissioners. Director Housman complimented Secretary Barbara Santos for her work on this matter. The Commissioners had various opinions. Commissioner Lyon indicated a preference to keep the current logo. He would like the logo to reflect the general aviation nature of the County's airports. Chairman Ballance suggested a business jet. Director Housman advised as to the need to avoid copyright issues. Commissioner Stewart noted that many of the airports do not have much use of jets. Chairman Ballance indicated that a business jet would reflect economic development. Commissioner Lyon suggested using current logo with deletion of "RC" in "RCALUC" and replacement of the aircraft model. Director Housman advised that smaller versions of the logo would be provided. Commissioner Holmes liked the angled runway, but would prefer the round logo. Commissioner Krupa likes the dynamic nature of the aircraft in the new logos. She agreed with Commissioner Holmes. Commissioner Stewart also agreed, but with a square logo. Commissioner Lyon suggested we table that discussion. Director Housman asked if there is agreement on circle vs square. Circle had 5 votes.

**II. 5.0 APPROVAL OF MINUTES**

The ALUC Commission by a vote of 5-0 approved the October 12, 2017 minutes. Abstained: Betts and Ballance



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III. **6.0 ORAL COMMUNICATION ON ANY MATTER NOT ON THE AGENDA**

Commissioner Lyon reminded the Commissioners of Veterans' Day festivities at Flabob. Chairman Ballance reminded of anniversary of Marine Corps and Veterans Day events.

IV. **7.0 COMMISSIONER'S COMMENTS**

None

V. **8.0 ADJOURNMENT**

Chairman Ballance adjourned the meeting at 10:33 A.M.

VI. **CD**

The entire discussion of this agenda item can be found on CD and referenced by the meeting time listed below. For a copy of the CD, please contact Barbara Santos, ALUC Commission Secretary, at (951) 955-5132 or E-mail at [basantos@rivco.org](mailto:basantos@rivco.org).

ITEM 4.0: TIME IS: 9:54 A.M.