

COUNTY OF RIVERSIDE AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM: 3-4 2.1

HEARING DATE: May 13, 2021 (Continued from ~~March 11~~ April 8, 2021)

CASE NUMBER: ZAP1444MA20 – Jared Riemer/PR III/CHI Freeway BC, LLC (Representative: Glassman Planning Associates, MG2, and MIG. Inc.)

APPROVING JURISDICTION: March Joint Powers Authority

JURISDICTION CASE NO: PP14-02 (Plot Plan/Determination of Substantial Conformance No. 2)

LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan (March ALUCP)

Airport Influence Area: March Air Reserve Base

Land Use Policy: Compatibility Zones B1-APZ-I, B1-APZ-II

Noise Levels: 65-75 CNEL

MAJOR ISSUES: ~~At the March 11, 2021, meeting, several new documents were submitted to the Commission. A detailed discussion of these documents is included in the staff report. Firstly, a multiple scenario intensity summary table was provided to the Commission by ALUC staff. The table identified several different scenarios on how to calculate the building's intensity for the Commission to consider in its decision-making process. Secondly, the applicant provided at the meeting a new floor plan increasing the breakroom room area from 1,500 square feet to 6,500 square feet. The applicant also provided an occupancy survey study from an existing similar facility, with the intentions on applying the study's occupancies numbers for the proposed 6,500 square foot breakroom area, in lieu of using the Building Code to calculate the breakroom occupancy. This floor plan was subsequently redesigned later and replaced with a 5,400 square foot breakroom area. Lastly, the Air Force provided email comments dated March 9, 2021, expressing concerns with the enforceability of the Covenant, and requested the project be continued so they can review the Covenant further.~~

On April 1, 2021, the applicant submitted a revised site plan clarifying the breakroom area as being 5,800 square feet gross (previously identified as 5,400 square feet), and the plan was introduced at the April 8, 2021, meeting. The applicant subsequently revised the entire floor plan on April 20, 2021, providing for several smaller break room areas that are spaced 210 feet apart to avoid each others single acre area. This revised floor plan has been reviewed in this staff report

and is consistent with the average and single acre intensity criteria for B1-APZ-I and B1-APZ-II per the Building Code. As such, the submitted survey occupancy study is no longer needed.

In addition, Air Force comments were received on April 6, 2021, and introduced at the April 8 meeting. The Air Force comments identified that the use of covenants to restrict building occupation in projects are encouraged by the Air Force, with the ultimate goal of updating the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan (ALUCP) so that the intensity criteria is consistent with the Department of Defense Instruction No. 4165.57 intensity criteria.

The Commission also discussed the potential of adopting a Hybrid Intensity Policy which would allow staff to review Parcel Delivery project's intensity by using a hybrid method of the building code and parking code. Several concerns were brought up at the meeting about adopting such a policy, such as whether or not an amendment to the Compatibility Plan would be needed to take such an action. The item was continued to the May 13 meeting for further discussion and review.

The project was continued to the May 13, 2021 meeting, to allow ALUC staff time to review and analyze the new floor plan intensity, update the staff report, and review the potential adoption of the hybrid intensity policy.

Since the April 8 meeting, ALUC staff, has completed its review and analysis of the potential policy adoption and had the following comments:

- ***Section 2.5.2 of the 2011 California Airport Land Use Planning Handbook (Handbook), states that procedures that the ALUC will use in reviewing local plans and project should be defined in the ALUCP.***
- ***The Handbook identifies several methods for determining usage intensities e.g. Building and Fire Codes, Facility Management Industry, Local Parking Standards, and Survey of Comparable Uses. The Handbook does not indicate or expressly allow the mixing of the various methods for determining usage intensities.***
- ***Appendix C of the Countywide Policies only identify three methods of calculating intensity: Building Code, Parking Code, and Survey of Similar Uses. The Countywide Policies do not expressly allow the mixing or blending of these methods to determine concentration of people.***
- ***In both the Handbook and Countywide Policies, examples of calculating occupancy shows these methodologies being calculated independently from each other.***
- ***Because a 'Hybrid Intensity' type of calculation is not defined in the Countywide Policies, and because the Handbook does not specifically allow this type of calculation, it is Staff's position that such calculation would be impermissible without changing the Countywide Policies.***
- ***Adopting such a policy would be an impermissible amendment to the ALUCP where the Handbook specifies the procedures for the ALUCP Amendments, under Section 2.4.2, and includes preparation of a draft ALUCP and environmental document, circulation for review and comment, noticing of a public hearing meeting, and adoption by resolution of the ALUCP and CEQA document.***

Therefore, staff recommends that the Commission NOT adopt the Hybrid Intensity Policy for the above reasons and remove the matter off-calendar. By not adopting the policy, the proposed Amazon Parcel Delivery Hub project intensity would now be reviewed using the Building Code method and Parking Code method independently. Such an analysis has been included in this staff report. In addition, as a result of the applicant's recent changes to the floor plan, the project's average and single acre intensities are now consistent with the B1-APZ-I and B1-APZ-II criteria with using just the Building Code method, and therefore no longer needs the policy to be found consistent.

~~The applicant is proposing revisions to the floor plans (use areas) of previously reviewed and subsequently approved projects (ZAP1107MA14 and ZAP1394MA19) located within the portions of Compatibility Zones B1-APZ-I and B1-APZ-II. A breakdown of use by Compatibility Zone indicates that the project's average and single acre intensity in Compatibility Zone B1-APZ-I and B1-APZ-II is inconsistent with the intensity criteria when using the Building Code Method. (based on staff's hybrid intensity methodology combining the building code method and parking code method, and assigning an high-cube occupancy to unidentified floor areas which is now identified on the site plan. Any variation to this methodology may result in a changed determination). As such, the applicant has prepared a survey occupancy study from an existing similar facility for the Commission to consider. The study identified a highest hourly total of 45 people for the breakroom area, and when used in conjunction with the remaining 26 people for the high-cube warehouse area, the project (in B1-APZ-II) results in a single acre intensity of 71 people, which is consistent with B1-APZ-II intensity criteria of 100 people. This conclusion is based on the Commission's acceptance of the occupancy survey study.~~

Although the proposed project is consistent with the ALUC's Compatibility Zone B1-APZ-I and B1-APZ-II single acre intensity criteria, both single acre intensities in APZ-I and APZ-II exceeds the Air Force's interpretation of Air Force Instruction 32-7063 dated December 18, 2015, which addresses Air Force policies on Land Use Compatibility in accordance with Department of Defense Instruction (DoDI) No. 4165.57, for APZ-I and APZ-II. The Air Force understands the DoDI as limiting intensity to a maximum of 25 people in any given acre in APZ-I and a maximum of 50 people in any given acre in APZ-II.

In order to address this issue, the applicant has executed and recorded a Covenant on the title of the property, restricting actual occupancy of the building to a maximum of 25 people in any given acre in APZ-I, and a maximum of 50 people in any given acre in APZ-II. Operation in compliance with this covenant will be necessary to satisfy Air Force and March Joint Powers Authority concerns. ~~regarding project intensity.~~

RECOMMENDATION: *Staff recommends that the Commission find the Plot Plan/Determination of Substantial Conformance CONSISTENT, subject to the conditions included herein. ~~Staff recommends that the Commission CONTINUE the matter to the April 8 May 13, 2021, meeting, pending completion of the Air Force review of the project.~~*

PROJECT DESCRIPTION: The applicant proposes to revise the occupancy use and floor plan of an existing (but vacant) 709,083 square foot high-cube industrial warehouse building to allow for a delivery parcel hub facility on 39.42 acres. There is no increase to the building's footprint. The building, as amended, would provide for ~~258,000~~ **307,000** ~~691,805~~ square feet of high-cube warehouse area, 8,000 square feet of office area, ~~1,500~~ **5,400 and 6,050** square feet of break room area (*six separate areas*). ~~126 loading/queuing van spaces, and 70-164 indoor van parking spaces.~~

On April 9, 2015, the Commission found the original project (ZAP1107MA14) consisting of 694,083 square feet of high-cube logistics warehouse, 12,000 square feet of first floor office area, and 3,000 square feet of second floor office mezzanine consistent with the March ALUCP. (That project also involved a General Plan Amendment and a Change of Zone.)

On January 9, 2020, the Commission found a subsequent revision to the original project (ZAP1394MA19) consisting of an additional 10,000 square feet of office area (by reducing warehouse area by the same square footage) also consistent with the March ALUCP.

PROJECT LOCATION: The site is located southerly of Alessandro Boulevard, easterly of Interstate 215, westerly of Old 215 Frontage Road, and northerly of Cactus Avenue within the land use jurisdiction of the March Joint Powers Authority, approximately 5,440 feet northwesterly of the northwesterly terminus of Runway 14-32 at March Air Reserve Base.

BACKGROUND:

Original Cases ZAP1107MA14 and ZAP1394MA19: The original project (ZAP1107MA14) proposed to construct a 709,083 square foot high-cube industrial warehouse building on 39.42 acres, and a subsequent revision (ZAP1394MA19) was proposed to revise the original building floor plan to provide for an additional 10,000 square feet of office area (by reducing warehouse area by the same square footage). Both projects were found consistent by the Commission.

It is important to note that both projects were consistent with the March Airport Land Use Compatibility intensity criteria, but also exceeded the intensity requirements for the Air Force's Department of Defense Instruction (DoDI) No. 4165.57 for APZ-I and APZ-II. This required the applicant to execute a Covenant, recorded on the title of the property, restricting the actual occupancy of the building to the DoDI intensity limits, which satisfied the Air Force and March Joint Powers Authority, and ultimately receiving ALUC consistency.

Multiple Scenario Intensity Summary Table: As introduced at the March 11, 2021, hearing, ALUC staff prepared a summary table which analyzed four different scenarios in calculating the building's intensity (table included in this staff report package). The intent of this table was to give the Commission an overview of the multiple way's building intensity could be calculated for this unique project.

After significant concerns were raised over the adoption of a Hybrid Intensity Policy at the April 8, 2021, meeting, such as whether or not an amendment to the Compatibility Plan would be needed to take such an action, ALUC staff conducted further research into the matter and had the

following comments:

- ***Section 2.5.2 of the 2011 California Airport Land Use Planning Handbook (Handbook), states that procedures that the ALUC will use in reviewing local plans and project should be defined in the ALUCP.***
- ***The Handbook identifies several methods for determining usage intensities e.g. Building and Fire Codes, Facility Management Industry, Local Parking Standards, and Survey of Comparable Uses. The Handbook does not indicate or expressly allow the mixing of the various methods for determining usage intensities.***
- ***Appendix C of the Countywide Policies only identify three methods of calculating intensity: Building Code, Parking Code, and Survey of Similar Uses. The Countywide Policies do not expressly allow the mixing or blending of these methods to determine concentration of people.***
- ***In both the Handbook and Countywide Policies, examples of calculating occupancy shows these methodologies being calculated independently from each other.***
- ***Because a ‘Hybrid Intensity’ type of calculation is not defined in the Countywide Policies, and because the Handbook does not specifically allow this type of calculation, it is Staff’s position that such calculation would be impermissible without changing the Countywide Policies.***
- ***Adopting such a policy would be an impermissible amendment to the ALUCP where the Handbook specifies the procedures for the ALUCP Amendments, under Section 2.4.2, and includes preparation of a draft ALUCP and environmental document, circulation for review and comment, noticing of a public hearing meeting, and adoption by resolution of the ALUCP and CEQA document.***

Therefore, staff recommends that the Commission NOT adopt the Hybrid Intensity Policy for the above reasons and remove the matter off-calendar. By not adopting the policy, the proposed Amazon Parcel Delivery Hub project intensity would now be reviewed using the Building Code method and Parking Code method independently. Such an analysis has been included in this staff report. In addition, as a result of the applicant’s recent changes to the floor plan, the project’s average and single acre intensities are now consistent with the B1-APZ-I and B1-APZ-II criteria with using just the Building Code method.

~~In the March 11, 2021, staff report, ALUC staff analyzed the project under the “Hybrid Methodology” scenario, which used a combination of Building Code and Parking Code methods like applying an occupancy of 1.0 persons per van space instead of the standard 1.5 person per space. However, this method did not account for the unidentified areas of the building as shown on site plan.~~

~~Per the Building Code and direction from the County’s Building Official, all floor areas are to be identified and assessed an occupancy. Therefore, the logic behind the “Hybrid Methodology 1: Unidentified Floor Area” scenario is the most applicable scenario for the project. This methodology uses the same parameters as the above-mentioned Hybrid Methodology, with the added inclusion of assigning a high-cube warehouse occupancy to the unidentified floor areas. As such, the applicant has revised their previous floor plan to show identifiable uses in these~~

~~floor areas i.e. high-cube warehouse area and van parking spaces. These recent floor plans revisions are encapsulated in the new (fifth) “Hybrid Methodology 2: All Floor Area Identified” in the table.~~

Non-Residential Land Use Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zones B1-APZ-I and APZ-II. Zone B1-APZ-I limits average intensity to 25 people per acre, and APZ-II limits average intensity to 50 people per acre. Approximately 29.15 acres are located within APZ-I and 12.8 acres within APZ-II.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan and the Additional Compatibility Policies included in the March ALUCP, the following rates were used to calculate the occupancy for the proposed project:

- High Cube Logistics Warehouse – 1 person per 1,428 square feet,
- Office – 1 person per 200 square feet, and
- Break Room – 1 person per 15 square feet,
- ~~Delivery van loading spaces/queuing spaces/van parking spaces~~ — 1 person per vehicle (Although it is common practice to apply the standard of 1.5 persons per vehicle using the parking code method, these vehicles are specifically delivery vans with no passengers unlike cars, therefore, it is reasonable to apply a ratio of 1 person per vehicle).

~~The proposed use, a parcel delivery hub, is a unique land use that includes regular pre-defined floor area uses like office and warehouse, as well as indoor van parking spaces, and van stacking and queuing spaces (used to load merchandise from the warehouse area into the vans). This project uniqueness has prompted ALUC staff to use a hybrid methodology to calculate intensity; using the building code method for establish rooms/areas like office and warehouse, and using the parking code method to calculate the occupancy of the indoor van parking spaces, and van loading/queuing spaces.~~

The proposed **amended** project would provide for ~~258,000~~ **307,000** ~~691,805~~ square feet of high-cube warehouse area, 8,000 square feet of office area, ~~1,500~~ **5,400** ~~and 6,050~~ square feet of break room area (*six separate areas*), ~~126~~ loading/queuing van spaces, and ~~70~~ **164** indoor van parking spaces, accommodating an occupancy of ~~518~~ **905** ~~928~~ people (which is **exceeds** the maximum allowed people in the building as specified in the existing recorded Covenant on the property), resulting in an average intensity of ~~13~~ **24** people per acre for the entire site, which is consistent with the Compatibility Zone B1-APZ-I **average intensity** criterion of 25, and B1-APZ-II criterion of 50.

A breakdown of use by Compatibility Zone indicates that Zone B1-APZ-I includes ~~166,325~~ **166,325** ~~513,530~~ square feet of high-cube warehouse area, **and 3,150 square feet of break room area**, ~~108~~ loading/queuing van spaces, and ~~70~~ **164** indoor van parking spaces ~~295~~ **389** accommodating ~~570~~ people, resulting in an average intensity of ~~10~~ **13** ~~20~~ people per acre for the portion of the site located in Zone B1-APZ-I, which is consistent with the Compatibility Zone B1-APZ-I average acre intensity criterion of 25.

Zone B1-APZ-II includes ~~91,675~~ **140,675** ~~178,275~~ square feet of high-cube warehouse area, 8,000 square feet of office area, ~~1,500~~ **5,400** and **2,900 square feet of break room area**, and ~~18~~ **loading/queuing van spaces** ~~223~~ **517** accommodating ~~358~~ people, resulting in an average intensity of ~~17~~ **40** ~~28~~ people per acre for the portion of the site located in Zone B1-APZ-II, which is consistent with the Compatibility Zone B1-APZ-II average acre intensity criterion of 50.

~~The project's average acre intensity is determined to be consistent, based on the hybrid methodology used by staff of combining the building code method and parking code method, as well as assigning a high-cube occupancy to unidentified floor areas now shown on the site plan. Any variation to this methodology may result in a changed determination.~~

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per standard vehicle and 1.0 persons per van delivery and trailer truck in the absence of more precise data). Based on the number of parking spaces provided (354 standard vehicles, 21 truck trailer, 576 van outdoor parking), the total occupancy would be estimated at 1,128 people for an average intensity of approximately 29 people per acre for the entire site, which is consistent with the Compatibility Zone B1-APZ-II criterion of 50, but inconsistent with the Zone B1-APZ-I criterion of 25.

It is important to note that the building's proposed occupancy, as calculated by the Building Code, results in ~~518~~ **905** ~~928~~ people, which is significantly less than the occupancy as calculated by the Parking Code Method (1,128 people). Commensurate with the nature of a parcel delivery hub, 65% of the project's total parking spaces are designated for delivery vans, which will have off-setting shifts, never resulting in all the van parking spaces being occupied at the same time. Therefore, in this instance, it is more realistic that the building occupancy would be closer to the occupancy generated by the Building Code Method, rather than the occupancy generated by the Parking Code Method.

Non-Residential Single-Acre Land Use Intensity: Compatibility Zones B1-APZ-I and APZ-II limit maximum single-acre intensity to 100 people. There are no risk-reduction design bonuses available, as March Air Reserve Base/Inland Port Airport is primarily utilized by large aircraft weighing more than 12,500 pounds.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre intensity in APZ-I includes **42,510 square feet of high-cube warehouse area and 1,050 square feet of break room area**, ~~52~~ **56** indoor van parking spaces for a total occupancy of ~~52~~ **56** ~~100~~ people, which is consistent with the Compatibility Zone B1-APZ-I single acre intensity criterion of 100.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre intensity in APZ-II includes a ~~1,500~~ **5,400** **42,510 square feet of high-cube warehouse area and 1,050 square feet of break room area**, ~~24,800~~ **24,579** (~~previously unidentified floor area~~) for a total occupancy of ~~100~~ **377** ~~100~~ people, which is consistent with the Compatibility Zone B1-APZ-I

single acre intensity criterion of 100.

The project's single acre intensity is determined to be ~~inconsistent in Zone B1-APZ-II~~, based on the hybrid methodology used by staff of combining the building code method and parking code method, ~~as well as assigning an high-cube occupancy to unidentified floor areas which is now shown on the site plan~~. Any variation to this methodology may result in a changed determination.

The project's average acre intensity is determined to be consistent, based on the hybrid methodology used by staff of combining the building code method and parking code method, as well as assigning a high-cube occupancy to unidentified floor areas. Any variation to this methodology may result in a changed determination.

Although the abovementioned single acre intensities in APZ-I and APAZ-II are ~~is~~ consistent with the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, **both single acre intensities in APZ-I and APZ-II are** inconsistent with the Air Force Department of Defense Instruction No. 4165.57 with regards to intensity, which is limited to a maximum of 25 people in any given acre in APZ-I, and 50 people in APZ-II. A more detailed analysis is provided below in the March Air Reserve Base section of the staff report.

Amazon Logistics – Occupancy Survey of Similar Facility: *It is important to note that with the applicant's revised floor plans, the project's average and single acre intensities are now consistent with the B1-APZ-I and B1-APZ-II criteria using the Building Code method, and therefore no longer needs to submit the supplemental occupancy survey study for the Commission's consideration.*

~~In light of the project's inconsistent single acre intensity (per the Building Code Method) in B1-APZ-II, the applicant prepared a survey study examining the occupancies of a similar parcel delivery facility.~~

~~The study highlights the fact that the building code method used for determining site intensity is a conservative approach in calculating actual project intensity. As such, the study examines one similar existing facility at 9350 Rayo Avenue, South Gate, for a more realistic occupancy generation calculation.~~

~~The study was taken between February 1, 2021, to March 2, 2021, and it identified the Rayo Avenue breakroom as having:~~

- ~~• highest daily total of 97 people,~~
- ~~• average daily total of 67 people, and~~
- ~~• highest hourly total of 45 people (taken from the survey days that could potentially exceed the single acre intensity criteria [when used in conjunction with the remaining 37,760 square feet of high-cube warehouse area accommodating 26 people], which includes data only available for one day February 1, February 7, February 8, February 11, February 12, and February 14).~~

Summary of Daily Counts: February 1st - March 2nd							
1-Feb	97	9-Feb	65	17-Feb	67	25-Feb	69
2-Feb	68	10-Feb	67	18-Feb	69	26-Feb	70
3-Feb	73	11-Feb	76	19-Feb	66	27-Feb	68
4-Feb	62	12-Feb	74	20-Feb	60	28-Feb	59
5-Feb	51	13-Feb	71	21-Feb	69	1-Mar	61
6-Feb	64	14-Feb	78	22-Feb	66	2-Mar	47
7-Feb	78	15-Feb	63	23-Feb	55		
8-Feb	74	16-Feb	60	24-Feb	73		

~~All of these results provide a significantly lower occupancy when compared to the proposed breakroom occupancy at the Alessandro Boulevard facility of 360-387 people, as calculated using the Building Code.~~

~~If we applied the highest hourly total from the study, 45 people, to the proposed 5,400-5,800 square foot breakroom at the Alessandro Boulevard facility, the project in the B1-APZ-II portion would accommodate a total occupancy of 590-212 people. More specifically, the intensity in B1-APZ-II would now result resulting in an average acre intensity of 18-17 people per acre, and a single acre intensity of 62-71 people (compared to 377-413 people using the Building Code method), both of which are consistent with the B1-APZ-II average and single acre intensity criteria of 50 and 100 respectively (there would be no changes to intensities in APZ-I).~~

~~The highest daily total of 97 people outlined in the survey study was not used in this calculation, as it does not accurately represent the highest number of people visiting the breakroom at any given time. Instead, it only identifies the total number of people entering the breakroom over the course of an entire day, and to use this number as an occupancy comparison would be unrealistic. Hourly occupancy data was provided, and it revealed a lower occupancy (per hour) than the daily total number.~~

~~It is important to note that the submitted survey study only counts the occupancy generated in the 4,720 square foot breakroom at the Rayo Avenue facility. It does not provide occupancy counts for the entire facility, which is what historically the Commission is accustomed to. The applicant is requesting that the Commission considers applying the occupancy counted at the Rayo Avenue breakroom facility and apply it to the proposed project's breakroom on Alessandro Boulevard.~~

~~In addition, the building size at the Rayo Avenue facility (211,356 square feet) is 30% smaller than the size of the proposed facility at Alessandro Boulevard (709,083 square feet), which may be considered incomparable. Lastly, the study only provides an hourly breakdown for just one day (February 1) of the 30-day survey collection. There is a potential that the hourly totals for the other unaccounted days may be higher than 45 people.~~

March Air Reserve Base/United States Air Force Input: Given that the project site is located in Zones B1-APZ-I and APZ-II of the primary runway at March Air Reserve Base, the March Air Reserve Base staff was notified of the project and sent a package of plans for their review. The Air Force provided their previous 2015 comments regarding the original shell building project (see

~~below). As of the time this staff report was prepared, we were still awaiting updated comments from the Air Force regarding this project. The Air Force letter dated March 10, 2015, included the following comments:~~

~~These comments have largely been addressed through the ALUC review and conditions. The only comment that is left unanswered is the Air Force's position on the proposed project with respect to permitted use and intensity i.e. if the Air Force accepts the existing executed Covenant as means of satisfying the AICUZ intensity criteria.~~

~~In addition, the Air Force provided email comments on March 9, 2021, outlining their concerns with the language of the Covenant agreement, specifically the enforceability of the Covenant regulations by the local jurisdictions. The Air Force has requested a continuance so they can review the Covenant and determine if any modifications are necessary.~~

The 2018 Airport Installation Compatible Use Zones (AICUZ) study identifies the project site as located within Accident Potential Zones I and II (APZ-I and APZ-II). Appendix A of the AICUZ provides Land Use Compatibility Tables for the APZs, which cite "warehousing" as permitted uses in APZ-I and APZ-II (and prohibited use in the Clear Zone [CZ]). The AICUZ does not explicitly identify a delivery parcel hub use.

However, March Air Reserve Base officials maintain that the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan is not consistent with current Air Force guidance found in Air Force Instruction 32-7063 dated December 18, 2015, which addresses Air Force policies on Land Use Compatibility in accordance with Department of Defense Instruction (DoDI) No. 4165.57. These inconsistencies include conflicts with regard to lot coverage, intensity, and permitted use definitions.

The proposed project complies with the restrictions on permitted uses and lot coverage, but not with the intensity limits. The Air Force understands the DoDI criteria as limiting intensity to a maximum of 25 people in any given acre in APZ-I and to a maximum of 50 people in any given acre in APZ-II. As noted above, the project would be expected to result in a single acre occupancy of ~~52~~ **100** people in APZ-I and a single acre occupancy of ~~100~~ **100** people in APZ-II.

The projected occupancy intensities would be inconsistent with the Air Force intensity understanding.

One method of bringing the project into consistency with both the Air Force Instruction (AFI) is for the applicant to agree to a condition including a Covenant, recorded on the title of the property, restricting the actual occupancy of the building to the limits of the AFI.

The **previous** applicant ~~has~~ agreed to this condition and has recorded and executed said document, which limits actual occupancy of the building in conformance with the limits of 25 and 50 persons, in any given acre within APZ-I and APZ-II, respectively. ~~(The project's proposed occupancy of 518 people does not exceed the occupancy stated in the Covenant, and therefore the Covenant language is still applicable).~~ Specifically, the existing Covenant states:

E. Covenanter has agreed to comply with the Density Restrictions and a Density Cap (both terms are defined below), by limiting occupancy of the Project to (i) five hundred and eighteen (518) occupants (“Density Cap”) **[THE DENSITY CAP WILL DECREASE IF THE SQUARE FOOTAGE OF THE BUILDING DECREASES.]**; (ii) twenty-five (25) occupants in any square area measuring 208 feet by 208 feet (“Square Area”) for all Square Areas within portions of the building of the Project within APZ I; and (iii) fifty (50) occupants in any Square Area within portions of the building of the Project within APZ II. Requirements (ii) and (iii) are collectively the “Density Restrictions”, and are depicted in Exhibit B, attached hereto and incorporated herein by reference. Accordingly, any building expansion is prohibited, including an increase in the building mezzanine area, without further review by the JPA and MARB representatives, and consent and approval provided through an amendment to this covenant.

~~The applicant has agreed to a condition to revise the Covenant language which addresses the Air Force concerns, as well as limiting actual occupancy of the building to 25 persons in any given acre within APZ-I and to 50 people persons in any given acre within APZ-II.~~

The Air Force provided a comment letter dated April 6, 2021, which was submitted at the April 8, 2021, ALUC meeting. The letter identified that the use of the Covenant was encouraged by the Air Force, with the ultimate solution being the completion of the March Air Reserve Compatible Use Study and the subsequent update to the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, which addresses the intensity inconsistency between the 2014 March ALUCP and the Department of Defense Instruction No. 4165.57.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zones B1-APZ-I and APZ II. Industrial warehouse buildings are compatible within Accident Potential Zones I and II pursuant to the 2018 Air Installation Compatible Use Zone (AICUZ) study disseminated by the United States Air Force. The AICUZ does not explicitly identify a delivery parcel hub use. Use as an industrial warehouse is also compatible pursuant to Department of Defense Instruction (DODI) No. 4165.57, but the intensity levels of this project in the absence of the Covenant would exceed DODI allowances, as understood by the Air Force.

The Air Force previously had concerns with the original project regarding uncovered water in the detention basins being a bird attractant source. Hazards to flight are prohibited in Compatibility Zones B1-APZ-I and APZ-II. However, these concerns were addressed with special ALUC drainage conditions designed to minimize the potential for the proposed basins to become bird attractants.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being in an area subject to aircraft noise in the 65-70 CNEL range. As a primarily industrial use not sensitive to noise (and considering typical anticipated building construction noise attenuation of approximately 20 dBA), the warehouse area would not require special measures to mitigate aircraft-generated noise. However, a condition is included to provide for adequate noise attenuation within office areas of the building so as to achieve an interior noise level of 45 CNEL.

Part 77: The elevation of Runway 14-32 at its northerly terminus is approximately 1535.1 feet above mean sea level (1,535.1 feet AMSL). At a distance of approximately 5,920 feet from the runway, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,594.3 feet AMSL. The apparent finished floor elevation of the building is approximately 1,541 feet AMSL. The proposed building has a maximum height of 44.3 feet for a potential maximum elevation of 1,585.3 feet AMSL. Therefore, review by the FAA Obstruction Evaluation Service would not normally be required. However, March Joint Powers Authority, the jurisdiction of record, requires submittal of Form 7460-1 for all building projects within their area. The original applicant submitted Form 7460-1, the FAA assigned Aeronautical Study No. 2015-AWP-566-OE, and a Determination of No Hazard letter was issued by the FAA OES on May 29, 2015. A new submittal (2018-AWP-11013-OE) was made in 2018, and FAA issued a Determination of No Hazard letter on July 16, 2018.

The proposed floor plan change does not alter the building height or the FAA's no hazard determination.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)

- (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, congregate care facilities, hotels/motels, places of assembly, restaurants, hazardous materials manufacture/storage (excluding storage of quantities of less than 6,000 gallons of flammable materials in the APZ II portion of the property), noise sensitive outdoor nonresidential uses, and hazards to flight.
 - (f) Retail trade, eating and drinking establishments, personal services, professional services, educational services, governmental services, medical facilities, cultural activities, and any other uses providing on-site services to the public.
 - (g) Commercial service uses; civic uses; churches, chapels, and other places of worship; classrooms; gymnasiums; theaters; conference or convention halls; auditoriums; fraternal lodges; gaming; auction rooms.
 - (h) Manufacturing of: food and kindred products, textile mill products, apparel, chemicals and allied products, rubber and plastic products, fabricated metal products, professional, scientific, and controlling instruments, photographic and optical goods, watches and clocks.
3. Prior to issuance of any building permits, the landowner shall convey and have recorded an avigation easement to the March Inland Port Airport Authority. Contact March Joint Powers Authority at (951) 656-7000 for additional information.
 4. The attached notice shall be given to all prospective purchasers of the property and/or tenants of the building. While not required, the applicant and its successors-in-interest are encouraged to provide a copy of said notice to employees who would regularly be working at this location.
 5. Any new detention basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the detention basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

6. In order to ensure proper functioning of the project drain system to avoid potential hazards to March Air Reserve Base flights, an additional Best Management Practice (BMP) shall be added to the project Water Quality Management Plan (WQMP). The applicant shall enter into a covenant and agreement with the March Joint Powers Authority similar to the Water Quality Management Plan and Urban Runoff BMP Transfer, Access and Maintenance Agreement between March Joint Powers Authority and Sun Life Assurance Company of Canada (Document No. 2014-0030862), which shall be recorded prior to issuance of a certificate of occupancy. A copy of the recorded agreement and BMP shall be provided to the Riverside County Airport Land Use Commission. The BMP shall include the following program:
 - a. The property owner (Proficiency 215 LLC or its successor(s)-in-interest, hereinafter "Owner") or its designated representative shall monitor the conditions of the detention basins and promptly inspect such basins following the completion of each "significant" rain event and the 48-hour period thereafter.
 - b. If any standing water remains in a basin that is not beneath a rock, gravel, or other layer following the completion of the "significant" rain event and the 48 hour period thereafter, Owner or its designated representative shall arrange to have such standing water either removed or covered within the next two business days following the conclusion of the 48 hour period.
 - c. In the event that the standing water situation recurs on a regular basis following the 48-hour detention period, the detention basin may no longer be draining as originally designed to prevent standing water from rising above a rock, gravel or other layer (for example, due to a rise in groundwater levels or other circumstance beyond Owner's ability to control). In that situation, Owner or its designated representative shall promptly engage a licensed civil engineer to prepare a design plan to assure that such condition does not persist for more than 48 hours following the conclusion of a "significant" rain event. The required engineering design solution shall be implemented promptly, but no later than 180 days following its approval by all applicable authorities, providing that, until such time as the engineered design solution is implemented, Owner or its designated representative will maintain water levels below the rock, gravel, or other layer.

(As amended by the Airport Land Use Commission on April 9, 2015)

7. This project has been evaluated as a proposal for ~~258,000-307,000~~ **691,805** square feet of high-cube warehouse area, 8,000 square feet of office area, ~~1,500-5,400~~ **and 6,050** square feet break room area (six separate areas). ~~126 loading/queuing van spaces, and 70-164 indoor van parking spaces.~~ Any increase in building area, change in use to any higher intensity use, change in building location, or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.
8. Zoned fire sprinkler systems shall be required throughout the building.
9. Office space must have sound attenuation features sufficient to reduce interior noise levels from exterior aviation-related sources to no more than CNEL 45 dB. March Joint Powers Authority shall require an acoustical study to ensure compliance with this requirement.
10. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
11. The project shall be in compliance with the recorded and executed Covenant, which limits building occupancy to a maximum of 25 people in any given acre in APZ-I and 50 people in any given acre in APZ-II.
12. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission and March Air Reserve Base.
13. The Federal Aviation Administration has conducted an aeronautical study of the proposed structure (Aeronautical Study No. 2018-AWP-11013-OE) and has determined that neither marking nor lighting of the structure is necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 L Change 1 and shall be maintained in accordance therewith for the life of the project.
14. The maximum height of the proposed structure (including any roof-mounted equipment) shall not exceed 48 feet above ground level, and the maximum elevation of the proposed structure at top point shall not exceed 1,585 feet above mean sea level.
15. The specific coordinates, height, and top point elevation of the proposed structure shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in structure height or elevation shall not require further review by the Airport Land Use Commission.

16. Temporary construction equipment used during actual construction of the proposed structure shall not exceed the height of the structure (48 feet), unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.
17. Within five (5) days after construction of the proposed structure reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to <https://oeaaa.faa.gov> for instructions.)
18. **The applicant has agreed to accept a Covenant which will be recorded on the title of the property restricting the actual occupancy of the buildings to the limits of the Air Force Instruction, and to the satisfaction of the Air Force. The project shall be in compliance with the recorded and executed Covenant, which limits building occupancy to a maximum of 25 people in any given acre in APZ-I, and a maximum of 50 people in any given acre in APZ-II. The Covenant shall include the following language:**

“Covenanter has agreed to comply with the Density Restrictions and a Density Cap (both terms are defined below), by limiting occupancy of the Project to (i) five hundred and eighteen (518) occupants (“Density Cap”) [THE DENSITY CAP WILL DECREASE IF THE SQUARE FOOTAGE OF THE BUILDING DECREASES.]; (ii) twenty-five (25) occupants in any square area measuring 208 feet by 208 feet (“Square Area”) for all Square Areas within portions of the building of the Project within APZ I; and (iii) fifty (50) occupants in any Square Area within portions of the building of the Project within APZ II. Requirements (ii) and (iii) are collectively the “Density Restrictions”, and are depicted in Exhibit B, attached hereto and incorporated herein by reference. Accordingly, any building expansion is prohibited, including an increase in the building mezzanine area, without further review by the JPA and MARB representatives, and consent and approval provided through an amendment to this covenant.”

Compliance shall be verified by MJPA or third-party inspections and reports on a schedule agreed upon by the applicant/project operator, the MJPA, and MARB representatives.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

THERE IS AN AIRPORT NEARBY.

**THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS**

**PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES**



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



**DEPARTMENT OF THE AIR FORCE
AIR FORCE RESERVE COMMAND**

April 6, 2021

Colonel Rodney E. McCraine
Commander
452d Mission Support Group
1261 Graeber Street, Bldg. 2313, Suite 135
March ARB, CA 92518

Mr. Paul Rull
Director
Riverside County Airport Land Use Commission
4080 Lemon Street, 14th Floor
Riverside, CA 92501

Dear Mr. Paul Rull,

This is in response to three applications, listed below, being considered by the Riverside County Airport Land Use Commission (ALUC) and addresses the application of Department of Defense Instruction (DODI) 4165.57 and Office Density Restriction Covenants for Riverside County Airport Land Use Commission (ALUC) on these cases –

- ZAP1444MA20 (Amazon Parcel Delivery Hub at Freeway Business Center, March JPA),
- ZAP1452MA21 (OZI Warehouse, City of Riverside),
- ZAP1456MA21 (Penske Truck Facility, City of Moreno Valley)

With respect to the 3 projects, and for the any future projects and applications, March ARB encourages the ALUC to follow the DODI 4165.57 as a minimum guide to determining office density restrictions and appropriate covenants to be used in ensuring that the density restrictions are met. The Instruction encourages local governments to address labor intensity, which addresses the allowable number of persons within a building and even in portions of the buildings. The most recent F-16 aircraft accident in March 2019 near March ARB is an example of the risk to property and life that exists when developments encroach upon an Air Base. Luckily, the F-16 crashed in a portion of the building with very low labor intensity and no one was injured. However, the accident could have easily happened only a few feet to the west where the higher labor intensity of the office space in that building could have resulted in serious bodily injury or death.

The ALUC should always consider the risk of an aircraft accident when making a decision. The use of Accident Potential Zones (APZ) described in the DODI 4165.57 address the general risk, but the DODI is only minimal guidance for use by local authorities to reduce the risk to the general public. There are two equally important aspects to the risk: the potential for aircraft to have an accident, and the potential for the public to be impacted by an accident. The Air Force (AF) is ready to work with ALUC to address March ARB risks and approaches to reduce the potential for accidental injury or death to the public both in and outside of allowable developments within the APZs. It is the Air Forces desire that the ALUC would side with conservative limits on building and development occupations.

The ALUC and local governments can do much to reduce potential accidents by having strong codes to reduce the cumulative development impact on air-pollution, electronic interference with aircraft, height of structures, and potential glare to pilots. Likewise, ALUC and local governments can greatly reduce the potential risk to the public by strong codes to reduce occupation density of land uses within the APZs, not only within certain land uses, but also labor intensity within building uses in the APZs.

ALUC's use of deed covenants to restrict building occupation in developments, which are not located in a city with adequate land use controls, is encouraged by the AF. The specific wording must ensure that the entity having jurisdiction be able to enforce the covenant. The AF is not able to inspect or enforce any deed covenants. This is a local government responsibility and authority. Covenants are not easily enforced and strict land use controls by the City and County are the real solution to not only reduce the risk but to enforce the reduced risk in the future.

Finally the AF will work with the ALUC in its Compatibility Use Study to develop a Compatibility Use Plan to develop further guidance for local governments to address and enforce; occupation density in APZs, labor intensity within buildings and other factors, to include the cumulative impacts of multiple building solar glare, air emissions, bird and wildlife hazards, etc. We look forward to working with the ALUC and its member governments in this regard.

Should you have any questions or would like to discuss this further, please feel free to contact me directly at (951) 655-3700.

Sincerely

MCCRAINE.RO Digitally signed by
MCCRAINE.RODNEY.ERI
DNEY.ERIC.10 C.1067944492
67944492 Date: 2021.04.05
15:55:27 -07'00'

RODNEY E. MCCRAINE, Colonel, USAF
Commander, 452d Mission Support Group

OFFICE (10 ASSEMBLY BAY)
4,000 GROSS SF
OFFICE = 200 SF / OCCUPANT
8,000 SF / 200 = 40 OCCUPANTS

BREAKROOM
200 SF GROSS
A-3 ASSEMBLY = 15 SF / OCCUPANT
800 SF / 15 = 53 OCCUPANTS

WAREHOUSE APZ II
178,275 GROSS SF
WAREHOUSE = 1428 SF / OCCUPANT
178,275 SF / 1,428 = 125 OCCUPANTS

BREAKROOM
1,050 SF GROSS
A-3 ASSEMBLY = 15 SF / OCCUPANT
1,050 SF / 15 = 70 OCCUPANTS

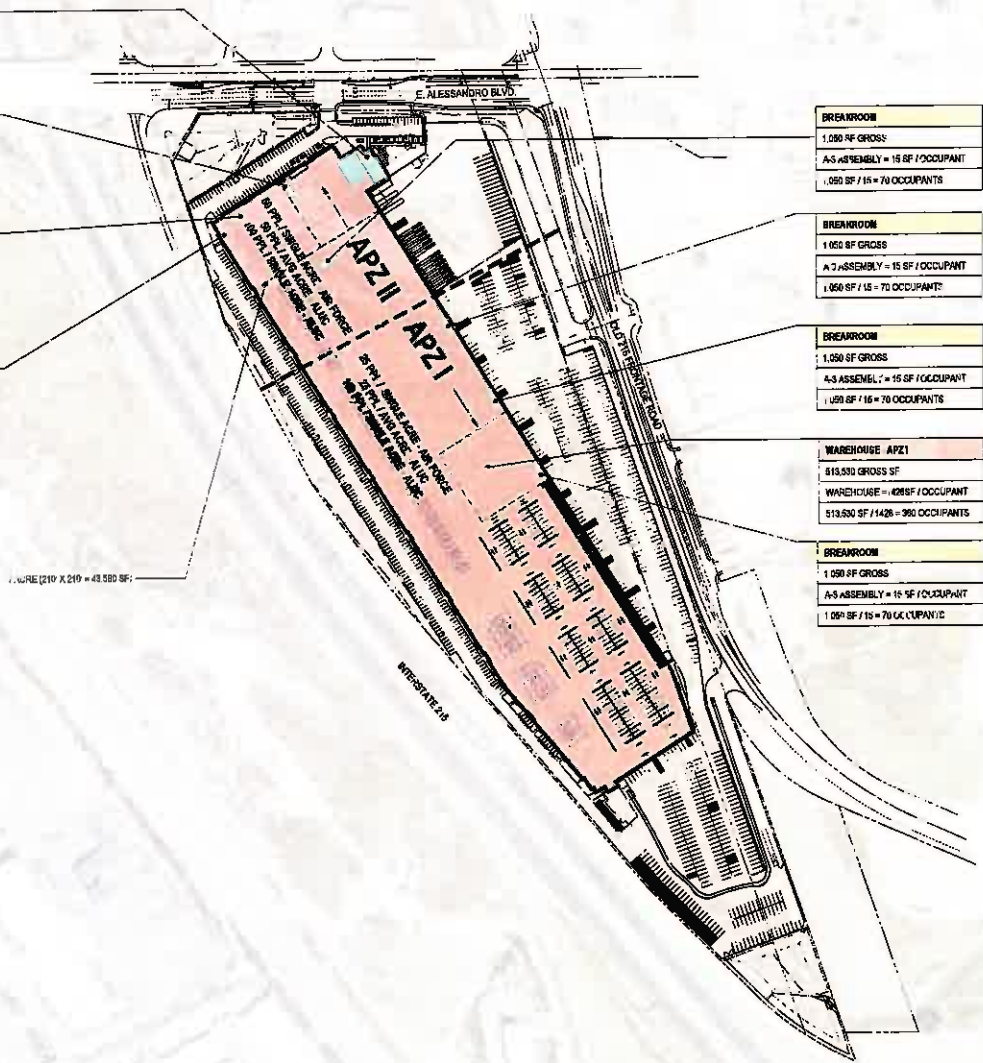
BREAKROOM
1,050 SF GROSS
A-3 ASSEMBLY = 15 SF / OCCUPANT
1,050 SF / 15 = 70 OCCUPANTS

BREAKROOM
1,050 SF GROSS
A-3 ASSEMBLY = 15 SF / OCCUPANT
1,050 SF / 15 = 70 OCCUPANTS

BREAKROOM
1,050 SF GROSS
A-3 ASSEMBLY = 15 SF / OCCUPANT
1,050 SF / 15 = 70 OCCUPANTS

WAREHOUSE APZ I
513,530 GROSS SF
WAREHOUSE = 1428 SF / OCCUPANT
513,530 SF / 1,428 = 360 OCCUPANTS

BREAKROOM
1,050 SF GROSS
A-3 ASSEMBLY = 15 SF / OCCUPANT
1,050 SF / 15 = 70 OCCUPANTS



PARKING BREAKDOWN

TOTAL ASSOCIATE STALLS (18' x 8')	340
TOTAL ACCESSIBLE PARKING STALLS	14
VAN PARKING (EXTERIOR)	576
VAN PARKING (INTERIOR)	70
TOTAL VAN PARKING STALLS (27' x 11')	646

OCCUPANCY BREAKDOWN

	APZ I (28.15 AC)		APZ II (12.8 AC)		TOTAL	
	AREA (SF)	OCCUPANTS	AREA (SF)	OCCUPANTS	AREA (SF)	OCCUPANTS
OFFICE	-	-	8,000	40	8,000	40
BREAK ROOM	3,150	210	2,900	193	6,050	403
WAREHOUSE (@ 1428 SF / PERSON)	513,530	360	178,275	125	691,805	485
TOTAL	516,680	670	189,175	358	705,855	828

AVG. PER ACRE (25 APZ I / 50 APZ II)	COMPLIES	18.6	COMPLIES	28
MAX SINGLE ACRE OCCUPANTS (100)	COMPLIES	100	COMPLIES	100



Architect contact: Kevin Marx
 Phone: 206.962.6483
 Email: Kevin.Marx@mg2.com



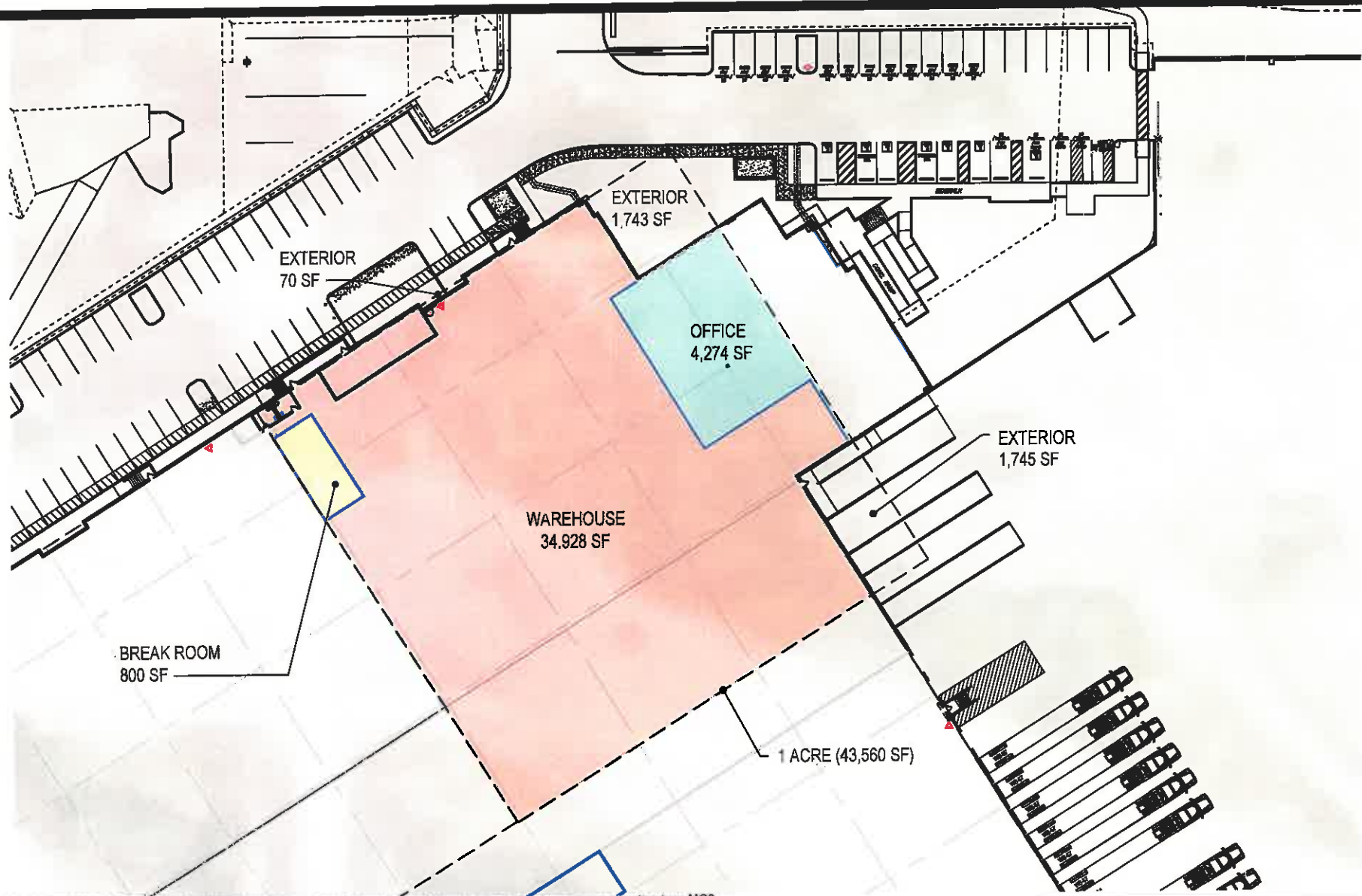
1" = 150'-0"

TI - FREEWAY BUSINESS CENTER

2677 E. Alessandro Blvd Riverside, CA 92508

APRIL 19, 2021

SITE PLAN



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 Ste 100
 Seattle, WA 98101

206.962.6500
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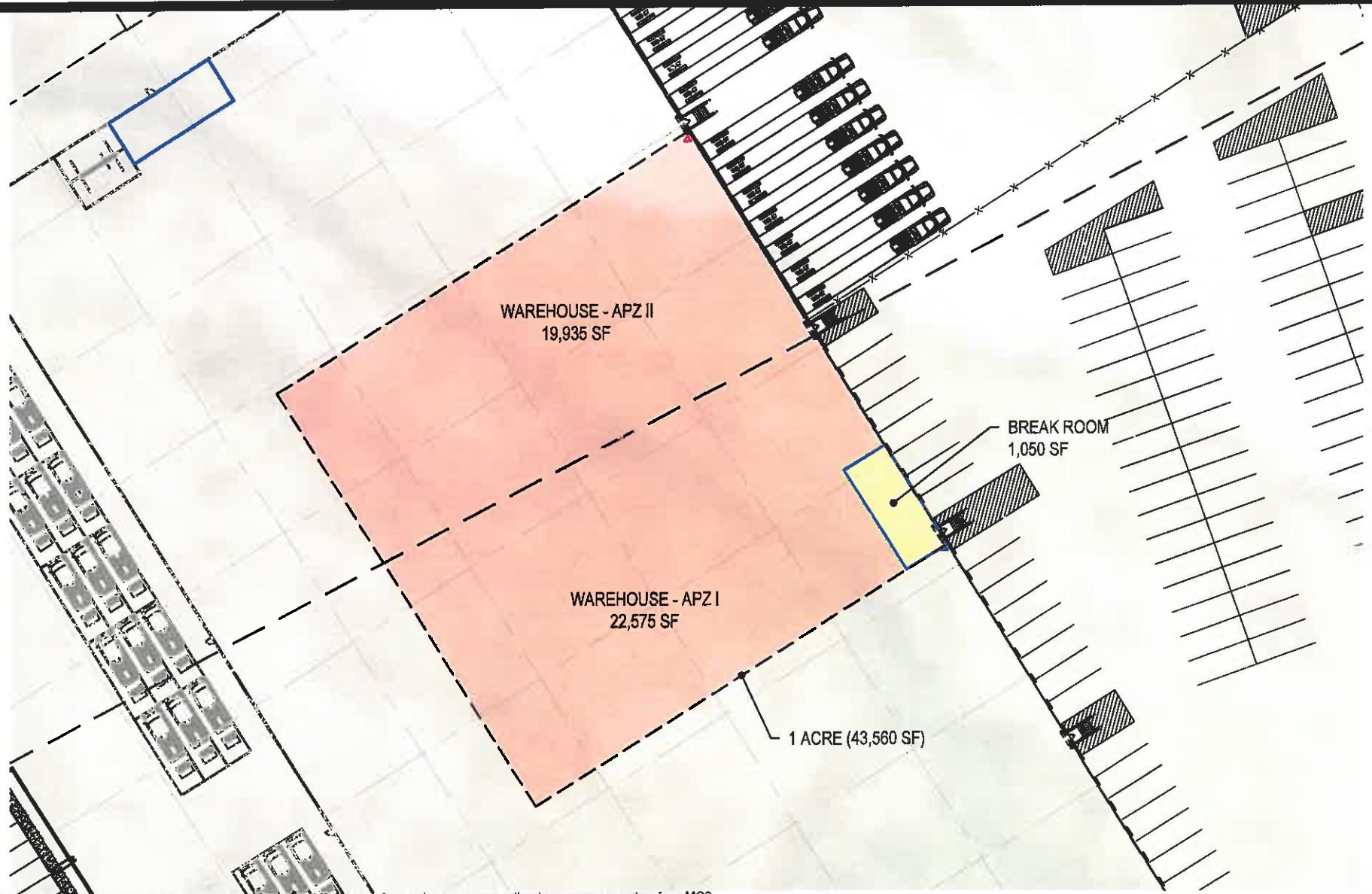


TI - FREEWAY BUSINESS CENTER

2677 E Alessandro Blvd Riverside, CA 92508

SINGLE ACRE DIAGRAM

20-5910-01
 PM: KM
 April 20, 2021



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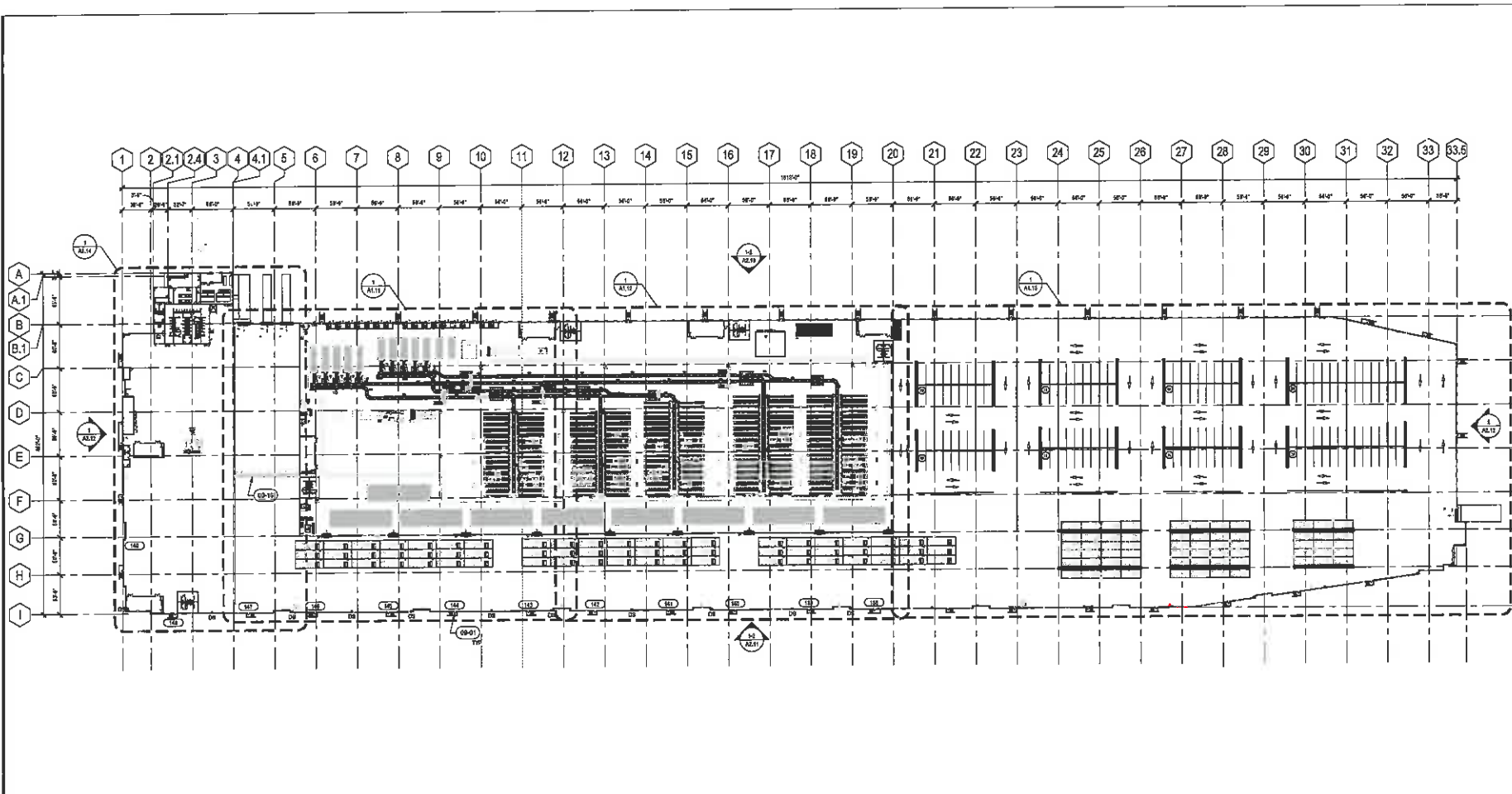
SINGLE ACRE DIAGRAM

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 PM: KM
 April 20, 2021

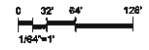


2677 E ALESSANDRO BLVD
RIVERSIDE, CA 92506

TENANT IMPROVEMENT FREEWAY BUSINESS CENTER 2677 E ALESSANDRO BLVD RIVERSIDE, CA 92508



1 OVERALL FLOOR PLAN
SCALE: 1/8" = 1'-0"



LEGEND	
(Symbol)	REWORK
(Symbol)	ANCHOR
(Symbol)	WALL TYPE TAG
(Symbol)	DOOR TYPE TAG
(Symbol)	WINDOW PERMITS REFER TO SHEET 44.6
(Symbol)	TURN IN/OUT PARTITION
(Symbol)	TELEPHONE WALL
(Symbol)	EXPANSION JOINT
(Symbol)	PANEL JOINT
(Symbol)	DOWN SPIGOT
(Symbol)	SHOULDER REFER TO C131 BUILDING BROWSE
(Symbol)	SPOCKLEYSER
(Symbol)	EXISTING DOOR TO REMAIN
(Symbol)	NEW DOOR
(Symbol)	PROPOSED SURFACE-MOUNTED FIRE EXTINGUISHER LOCATIONS

KEY NOTES	
29-16	PROESTRIM CROWNLINE AT FINISH FLOOR LEVEL
29-29	DOOR CHARGES REFER TO DETAIL 148.1.24

GENERAL NOTES	
A.	ALL EXTERIOR WALLS TO BE TRIM-FINISH PANEL. USE EXISTING UTI-ROUGE
B.	REFER TO STRUCTURAL GENERAL NOTES ON G.I. FOR ADDITIONAL PERFORMANCE
C.	REFER TO SHEET 44.6 FOR WALL TYPES
D.	REFER TO SHEET 44.6 FOR DOOR TYPES
E.	DETAILS NOT PROVIDED IN THIS SET SHALL BE APPLIED TO ALL MEMBERS UNLESS THE MAIN CONDITION SPECIFIC CALLOUTS ARE PROVIDED
F.	MEMBERS ARE OPEN TO THE COLUMN GRID. CASE OF SPIN, CENTERLINE OF COLUMN OR MEMBER CENTERLINE IS SHOWN BY DOTTED LINE. CENTERLINE OF MEMBER IS SHOWN BY SOLID LINE.
G.	MEMBER CONNECTIONS TO VERIFY AND CONFIRM ALL DIMENSIONS AND CONDITIONS. VERIFY CONNECTIONS TO CONFIRM ALL DIMENSIONS AND CONDITIONS. VERIFY CONNECTIONS TO CONFIRM ALL DIMENSIONS AND CONDITIONS.
H.	REFER TO SHEET 44.6 FOR ANCHORS AND SPANNCES
I.	ALL EXTERIOR WALLS - PORTABLE FIRE EXTINGUISHERS ARE PROVIDED AS REQUIRED BY IBC, REFER TO PLAN 148.1.1 FOR GENERAL LAYOUT FOR REFERRED. REFER TO THE PROTECTION DRAWINGS FOR FIRE ALARMS, TYPIE AND LOCATIONS
J.	PORTABLE FIRE EXTINGUISHER LOCATIONS SHALL BE DETERMINED BY THE DEPARTMENT FIELD INSPECTION IN ACCORDANCE WITH FIRE CODE.

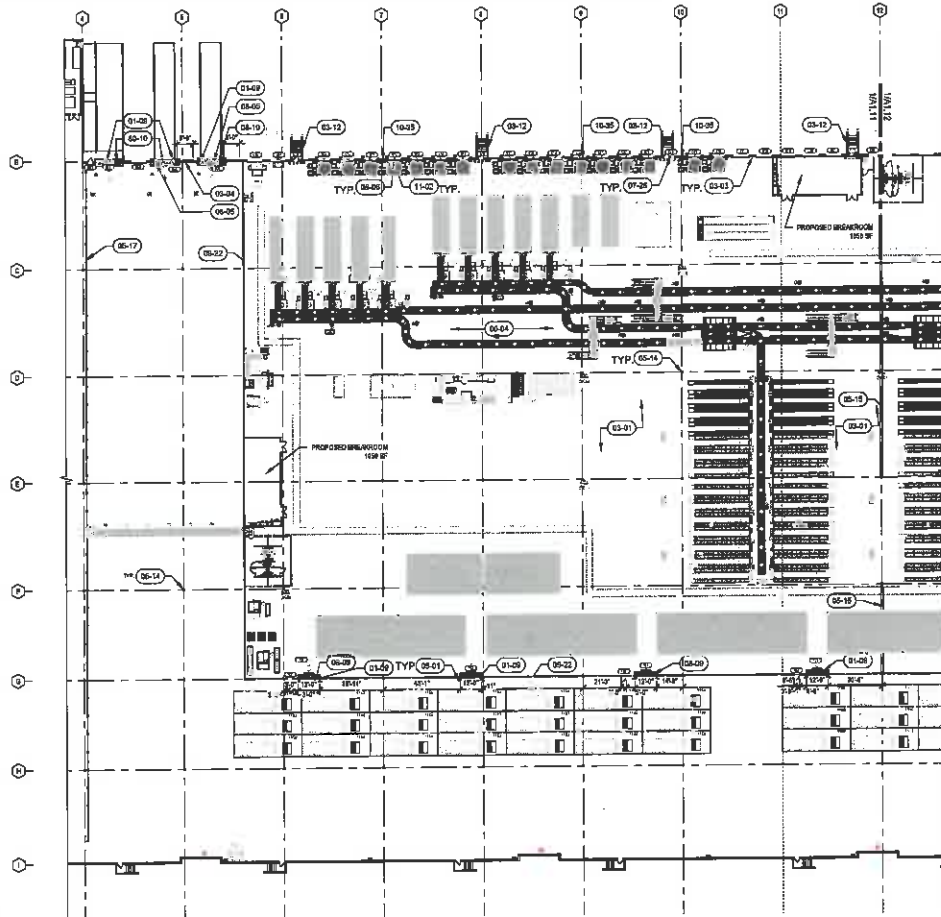
REVISION	DATE
ALIC PC Rev 001	10/20/2010
ALIC PC Rev 002	05/20/2011
ALIC PC Rev 003	03/29/2011
ALIC PC Rev 004	09/20/11
ALIC PC Rev 005	09/12/2011

PROJECT STATUS	
JOB NUMBER	20-6810-01
DRAWN BY	BSJ
CHECKED BY	KM
SHEET TITLE	OVERALL FLOOR PLAN
SHEET NUMBER	A1.10



2577 E ALESSANDRO BLVD
RIVERSIDE, CA 92508

TENANT IMPROVEMENT
FREEMWAY BUSINESS CENTER
2677 E ALESSANDRO BLVD
RIVERSIDE, CA 92508



1 PARTIAL FLOOR PLAN - WAREHOUSE
SCALE: 1/32"=1'-0"

KEYPLAN	LEGEND
	EXISTING WALL
	WALL TO BE REMOVED
	DOOR TO BE REMOVED
	INTERIOR PARTITION (REFER TO SHEET A1.10)
	WALL TO BE REMOVED
	TIILT UP WALL
	EXPANSION JOINT
	PANEL DOOR
	DOOR TRACK
	DOUBLE DOOR (REFER TO C1.21) (SEE ALSO DOUBLE DOOR)
	DOCK LEVELER
	DOOR TO REMAIN
	NEW DOOR

KEY NOTES
20-04 MATERIAL HANDLING EQUIPMENT (MHE) ROOM FOR REFERENCE
01-00 CLEAR ZONE DEMARKED BY PAINTED RED STRIPES ON THE GROUND WITH (ASGAL) STRIPING IN CENTER AREA. CLEAR ZONE TO BE THE WIDTH OF THE OPENING AND 6'-6" DEEP (SEE 2'-0" TO BOTH THE INTERIOR AND EXTERIOR OF THE DOOR). PROVIDE 6" ENCLOSED TEXT "STAY CLEAR" AT INTERIOR AND EXTERIOR EDGES OF EACH ZONE. TYP @ ALL HIGH-SPED ROLL UP DOORS
03-01 EXISTING REINFORCED CONCRETE SLAB, TYP.
03-03 EXISTING CONCRETE TILT UP WALL PANELS. PAINT PER ELEVATIONS
03-04 NEW CONCRETE INFILL. REFERENCE STRUCTURAL PAINT TO MATCH ADJACENT
05-12 EXISTING CONCRETE STAIR TO REMAIN, U.O.A.
05-01 NEW SURFACE MOUNTED BOLLARD
05-14 EXISTING COLUMN. GRAY PRIMER, PAINT SAFETY YELLOW TO 12' AFF.
05-15 STRUCTURAL BRACE FRAME TO REMAIN TYP.
05-17 GUARDRAIL (F.O.D.-FTT)
21-02 CHAIN LINK FENCE ENCLOSURE
27-26 RIGID INSULATION (MIN R-19) MOUNTED TO INSIDE FACE OF TILT UP WALL. INSTALL STARTING AT 10' AFF TO UNDERSIDE OF ROOF DECK.
28-06 NEW OVERHEAD 12V-14 DRIVE IN DOOR. MATCH EXISTING
28-07 EXISTING SECTIONAL OVERHEAD DOCK DOOR TO REMAIN. INSTALL NEW BUMPER, DOCK SEAL, DOCK LOCK AND MISC. DOCK EQUIPMENT PER DOOR SCHEDULE. REFER TO C1.21 FOR SIGNAGE DETAILS
28-20 NEW HIGH SPEED FABRIC ROLL UP DOOR (INTERIOR MOUNT). INSTALL NEW AIR CURTAIN ABOVE LOAD OUT DOOR. REF. MECHANICAL
28-11 HIGH SPEED FABRIC ROLL UP DOOR (EXTERIOR MOUNT)
28-22 NEW FULL HEIGHT DEMISING WALL
10-30 GC TO PROVIDE 24" WIDE WALL MOUNTED SHELVING FOR NEW ACCUMULATOR BOXES. MOUNT SHELVES AT 21' AFF. GC TO CONFIRM THERE ARE NO CONFLICTS WITH THE EXISTING OVERHEAD DOOR TRACK. REFERENCE PLUMBING DRAWINGS FOR ADDITIONAL INFORMATION. SHELVES MUST SUPPORT 1000 LB MIN.
11-02 NEW HYDRAULIC DOCK LEVEL. REFERENCE STRUCTURAL FOR SLAB REINFORCING DETAILS

GENERAL NOTES
1. EXISTING TILT-UP CONCR. PANEL, U.O.A.
2. DOOR SYMBOL KEY: SEE A4.19 FOR DOOR SCHEDULE
3. ELECTRICAL TO MEET CBC 2019
4. ALL FINISH MATERIALS SHALL MEET THE CLASS C FLOOR SPENDING INDEX OF 70-80 AND INTERIOR SPENDING INDEX OF 24-30 PER CBC 2019
5. DOORS INTO ELECTRICAL CONTROL PAINT ROOMS SHALL BE MARKED WITH A PLAINLY VISIBLE AND LEGIBLE MARK (SEE "ELECTRICAL ROOM" ON SCHEDULE WORKING)
6. FIRE PROTECTION EQUIPMENT (EDGE CONTAINING SPRINKLER BASES, PAID, OR OTHER SUPPRESSION OR CONTROL ELEMENTS) SHALL BE IDENTIFIED WITH APPROVED SIGNS. SIGNS SHALL BE CONTRACTED BY CULPABLE MATERIALS. PERMANENTLY MOUNTED AND LEADLY VISIBLE PER CBC 2019
7. SEE ALSO SHEET A1.10 FOR PARTITION TYPES
8. SEE ALSO SHEET A1.10 FOR OFFICE BLOCK WALL TYPES

DESCRIPTION	QTY	DATE
ALIC PC RUMBLE	1	10/20/2019
ALIC PC RUMBLE	1	01/16/2021

PROJECT STATUS	
JOB NUMBER	20-5810-01
DRAWN BY	SRJ
CHECKED BY	KM
SHEET TITLE	
LOADING AREA	
PARTIAL FLOOR PLAN	
SHEET NUMBER	
A1.11	

DATE PLOTTED: 11/06/2020 10:45 AM

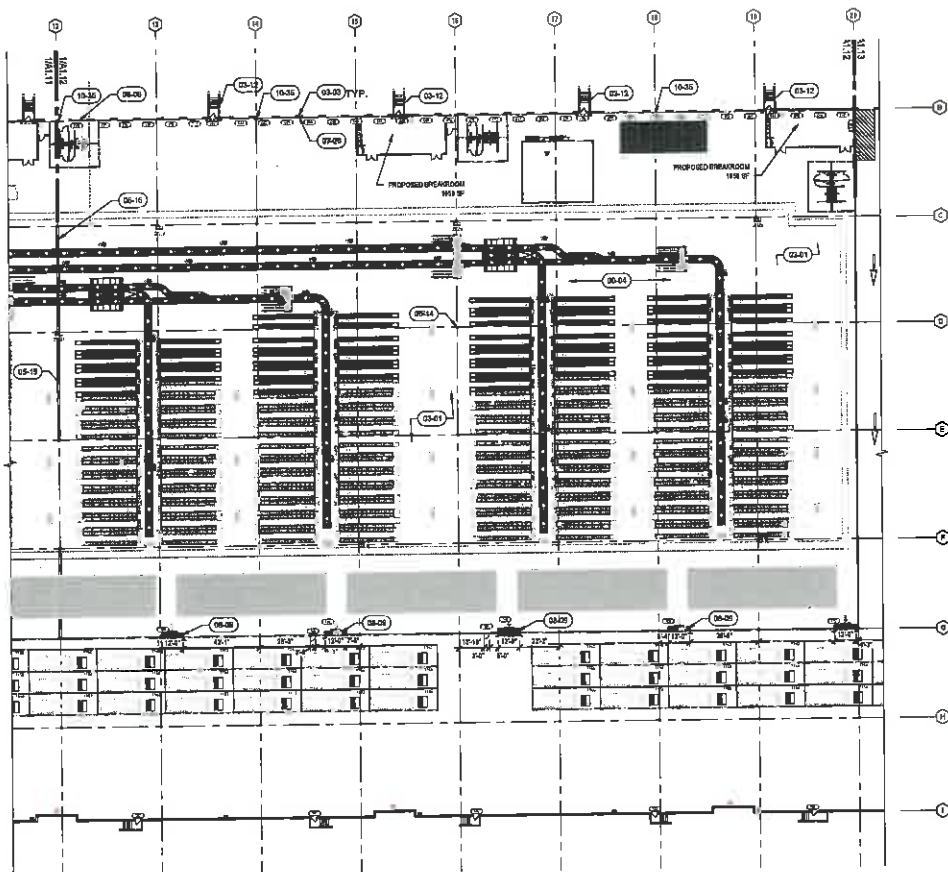
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DESCRIPTION	QTY	DATE
ALUMI PC Structural		12/20/18
ALUMI PC Structural		11/16/18
ALUMI PC Structural		11/16/18
ALUMI PC Structural		08/29/18
ALUMI PC Structural		04/10/18

PROJECT STATUS

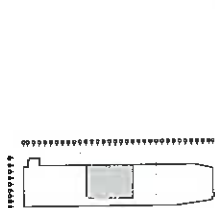
JOB NUMBER	20-8915-01
DRAWN BY	BSJ
CHECKED BY	KJM
SHEET TITLE	LOADING AREA PARTIAL FLOOR PLAN
SHEET NUMBER	A1.12

A1.12



1 PARTIAL FLOOR PLAN - WAREHOUSE
SCALE: 1/32"=1'-0"

KEY PLAN



LEGEND

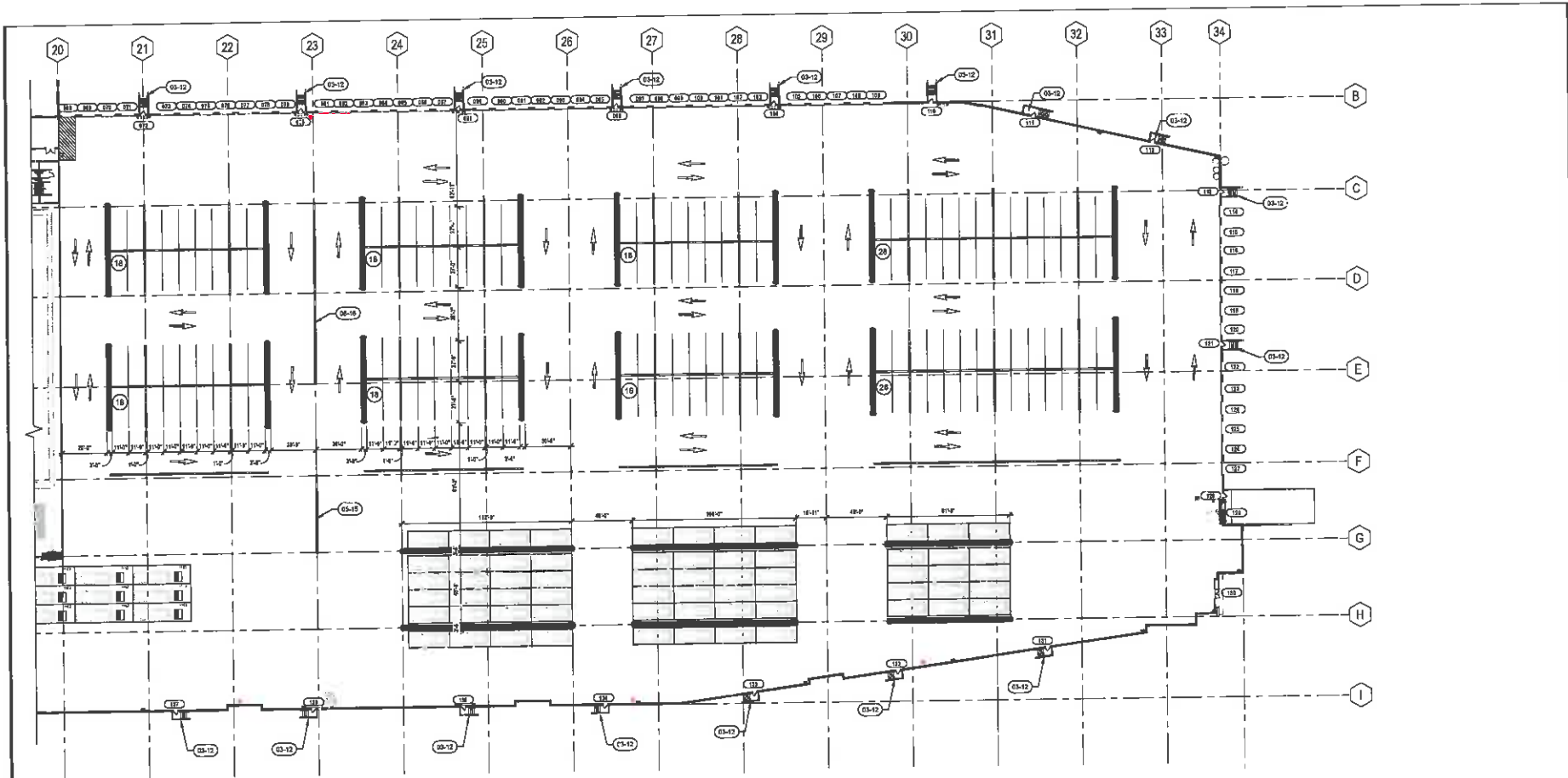
- REMOVE
- WALL
- WALL TYPE 106
- DOOR TYPE 108
- DIVISION PARTITION REFER TO SHEET A1.11
- 1/2" Gypsum PARTITION
- TILT PANEL WALL
- EXTENSION JOINT
- PANEL JOINT
- DOWN BRACKET
- REMOVE REFER TO EXIST BUILDING SURFACE
- DOCK LEVELER
- REMOVE DOOR TO REMAIN
- NEW DOOR

KEY NOTES

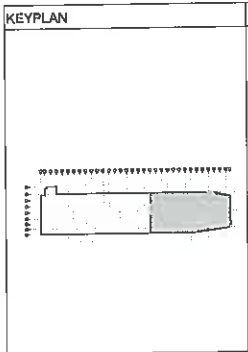
- 05-04 MATERIAL HANDLING EQUIPMENT (MHE) STORAGE FOR REFERENCE
- 01-09 CLEAR ZONE DEMARCATED BY PAINTED RED STRIPES ON THE GROUND WITH DIAGONAL STRIPING IN CENTER AREA. CLEAR ZONE TO BE THE WIDTH OF THE OPENING AND 4' CLEAR OFFSET 2' TO BOTH THE INTERIOR AND EXTERIOR OF THE DOOR. PROVIDE STENCILED TEXT "STAY CLEAR" AT INTERIOR AND EXTERIOR EDGES OF EACH ZONE, TYP @ ALL HIGH-SPEED ROLL UP DOORS
- 03-01 EXISTING REINFORCED CONCRETE SLAB, TYP.
- 03-03 EXISTING CONCRETE TILT UP WALL PANELS. PAINT PER ELEVATIONS
- 03-04 NEW CONCRETE IN-FILL. REFERENCE STRUCTURAL PAINT TO MATCH ADJACENT
- 03-12 EXISTING CONCRETE STAIR TO REMAIN, U.O.N.
- 05-01 NEW SURFACE MOUNTED BOLLARD
- 05-14 EXISTING COLUMN. GRAY PRIMER, PAINT SAFETY YELLOW TO 12' AFF.
- 05-15 STRUCTURAL BRACE FRAME TO REMAIN, TYP.
- 05-22 CHAIN LINK FENCE ENCLOSURE
- 07-20 RIGID INSULATION (MIN R-18) MOUNTED TO INSIDE FACE OF TILT UP WALL. INSTALL STARTING AT 1" AFF TO UNDERSIDE OF ROOF DECK.
- 06-06 NEW OVERHEAD 12V14 DRIVE IN DOOR. MATCH EXISTING
- 06-06 EXISTING SECTIONAL OVERHEAD DOCK DOOR TO REMAIN. INSTALL NEW SUMPERS, DOOR SEAL, DOCK LOCK AND MISC. DOCK EQUIPMENT PER DOOR SCHEDULE. REFER TO C1.21 FOR SIGNAGE DETAILS
- 06-06 NEW HIGH SPEED FABRIC ROLL UP DOOR (INTERIOR MOUNT). INSTALL NEW AIR CURTAIN ABOVE LOAD OUT DOOR, REF. MECHANICAL
- 05-10 HIGH SPEED FABRIC ROLL UP DOOR (EXTERIOR MOUNT)
- 09-22 NEW FULL HEIGHT DIVISION WALL
- 10-35 GC TO PROVIDE 24" WIDE MALL MOUNTED SHIELDING FOR NEW ACCUMULATOR BOXES. MOUNT SHIELDS AT 24" A.F.P. GC TO CONFIRM THERE ARE NO CONFLICTS WITH THE EXISTING OVERHEAD DOOR TRACK. REFERENCE PLUMBING DRAWINGS FOR ADDITIONAL INFORMATION. SHIELDS MUST SUPPORT SALES MHE.
- 11-03 NEW HYDRAULIC DOCK LEVELER. REFERENCE STRUCTURAL FOR SLAB REINFORCING DETAILS

GENERAL NOTES

1. EXISTING TILT-UP CONCRETE PANEL, U.O.N.
2. DOOR SYMBOL REF. SEE AS.10 FOR DOOR SCHEDULE
3. ELECTRICAL TO MEET CBC 2018
4. ALL FINISH MATERIALS SHALL MEET THE CLASS C FLOOR FINISH REQUIREMENTS OF 36-209 AND SHALL DEVELOPED UNDER OF 0-459 PER CBC 218
5. DOORS INTO ELECTRICAL CONTROL PANEL ROOMS SHALL BE MARKED WITH A PLAINLY VISIBLE AND DURABLE SIGN STATING "ELECTRICAL ROOM" OR SIMILAR MARKING
6. FIRE PROTECTION EQUIPMENT ROOMS CONTAINING SPRINKLER RISERS, VALVES, OR OTHER SUPPRESSION OR CONTROL DEVICES SHALL BE IDENTIFIED WITH APPROVED SIGNS. SIGNS SHALL BE CONSTRUCTED OF DURABLE MATERIALS, LEAD AND READILY VISIBLE PER CBC 218.9
7. SEE AS.15 FOR PARTITION TYPES
8. SEE ALSO SHEET A1.10 FOR OFFICE BLOCK WALL TYPES




1 PARTIAL FLOOR PLAN - WAREHOUSE
 SCALE: 1/32"=1'-0"



LEGEND

	WINDOW
	ENCLOSURE
	WALL TYPE 2/3
	INTERIOR PARTITION REFER TO SHEET 44-10
	1/4 HR FIRE PARTITION
	WALL PANEL WALL
	EXPANSION JOINT
	PANEL JOINT
	DOOR EGRESS
	STORAGE REFER TO CLS1 RAILING DRAWING
	DOCK LEVELER
	EXISTING DOOR TO REMAIN
	NEW DOOR

KEY NOTES

- 03-04 MATERIAL HANDLING EQUIPMENT, (MHE) SHOWN FOR REFERENCE.
- 01-09 CLEAR ZONE DEMARCATORY BY PAINTED RED STRIPING ON THE GROUND WITH DIAGONAL STRIPING IN CENTER AREA. CLEAR ZONE TO BE THE WIDTH OF THE OPENING AND 4'-0" DEEP. OFFSET 7'-0" TO BOTH THE INTERIOR AND EXTERIOR OF THE DOOR. PROVIDE STENCILED TEXT "STAY CLEAR" AT INTERIOR AND EXTERIOR EDGES OF EACH ZONE. TYP @ ALL HIGH-SPEED ROLL UP DOORS.
- 03-01 EXISTING REINFORCED CONCRETE SLAB, TYP.
- 03-03 EXISTING CONCRETE TILT UP WALL PANELS, PAINT PER ELEVATIONS.
- 03-04 NEW CONCRETE INFILL, REFERENCE STRUCTURAL PAINT TO MATCH ADJACENT.
- 03-12 EXISTING CONCRETE STAIR TO REMAIN, U.O.N.
- 03-01 NEW SURFACE MOUNTED ROLLARD.
- 03-14 EXISTING COLUMN GRAY PRIMER, PAINT SAFETY YELLOW TO 12' AFF.
- 03-18 STRUCTURAL BRACE FRAME TO REMAIN, TYP.
- 03-17 NEW ROUND-RAIL, FURNISHED AND INSTALLED BY TENANT.
- 03-23 CHAIN LINK FENCE ENCLOSURE.
- 03-06 NEW OVERHEAD 12x14' DRIVE IN DOOR, MATCH EXISTING.
- 03-06 EXISTING SECTIONAL OVERHEAD DOCK DOOR TO REMAIN, INSTALL NEW RUMPFER, DOCK SEAL, DOCK LOCK AND MISC. DOCK EQUIPMENT PER DOOR SCHEDULE. REFER TO CLS1 FOR SCHEDULE DETAILS.
- 03-09 NEW HIGH SPEED FABRIC ROLL UP DOOR (INTERIOR MOUNT), INSTALL NEW AIR CURTAIN ABOVE LOAD OUT DOOR, REF. MECHANICAL.
- 03-10 HIGH SPEED FABRIC ROLL UP DOOR (EXTERIOR MOUNT).
- 03-22 NEW FULL HEIGHT DENISING WALL.
- 11-03 NEW HYDRAULIC DOCK LEVEL, REFERENCE STRUCTURAL FOR SLAB REINFORCING DETAILS.

GENERAL NOTES

- EXISTING TILT-UP CONC. PANEL, U.O.N.
- DOOR SYMBOL REF: SEE AS-10 FOR DOOR SCHEDULE.
- ELECTRICAL TO MEET CMC 805P.
- ALL FINISH MATERIALS SHALL MEET THE CLASS C FLAME SPREAD INDEX OF 75 OR LESS AND SPRINKLER DEVELOPED INDEX OF 0-45 PER CMC 202B.
- DOORS INTO ELECTRICAL CONTROL BUILDING SHALL BE SPRAYED WITH A FLAMMABLE INHIBITING AND LOGIC DOOR STATING TO ELECTRICAL ROOM OR SIMILAR WORKING.
- FIRE PROTECTION EQUIPMENT ROOMS CONTAINING CONTROL PANELS SHALL BE IDENTIFIED WITH APPROVED SIGNING. SIGNS SHALL BE CONSTRUCTED OF DURABLE MATERIAL, REFINISHMENT, MOUNTED AND READILY VISIBLE PER CFC 201A.
- SEE AS-15 FOR PARTITION TYPES.
- SEE ALSO SHEET #A.10 FOR OFFICE BLOCK WALL TYPES.



2677 E ALESSANDRO BLVD
 RIVERSIDE, CA 92508

**TENANT IMPROVEMENT
 FREeway BUSINESS CENTER**
 2677 E ALESSANDRO BLVD
 RIVERSIDE, CA 92508

DESCRIPTION	QTY	UNIT
MALIC PC Roundhead	1	10455251
MALIC PC Roundhead	1	3453251
MALIC PC Roundhead	1	3453251
MALIC PC Roundhead	1	492321
MALIC PC Roundhead	1	631021

PROJECT STATUS

JOB NUMBER	20-5816-01
DRAWN BY	BSJ
CHECKED BY	KM

SHEET TITLE

LOADING AREA
PARTIAL FLOOR PLAN

SHEET NUMBER



2077 E ALESSANDRO BLVD
RIVERSIDE, CA 92508

**TENANT IMPROVEMENT
FREEWAY BUSINESS CENTER**
2677 E ALESSANDRO BLVD
RIVERSIDE, CA 92508

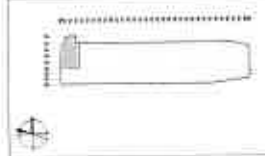
KEYNOTES

- 03-09 EXISTING CONCRETE TILT UP WALL PANELS, PAINT PER ELEVATIONS.
- 05-17 NEW GUARDRAIL.
- 08-05 NEW OVERHEAD 12'x4' DRIVE IN DOOR, MATCH EXISTING.
- 08-10 NEW HIGH SPEED FABRIC ROLL UP DOOR (EXTERIOR MOUNT).
- 09-22 NEW FULL HEIGHT FIRE EXTINGUISHING WALL.
- 11-01 NEW PRE-FABRICATED DOCK RAMP.

LEGEND

- NOTE
- OUTLINE
- EXTERIOR PARTITION, REFER TO SHEET 03-13
- 1/2\"/> PARTITION
- EXISTING TILT WALL PANEL
- EXISTING DOOR TO REMAIN
- NEW DOOR

KEY PLAN

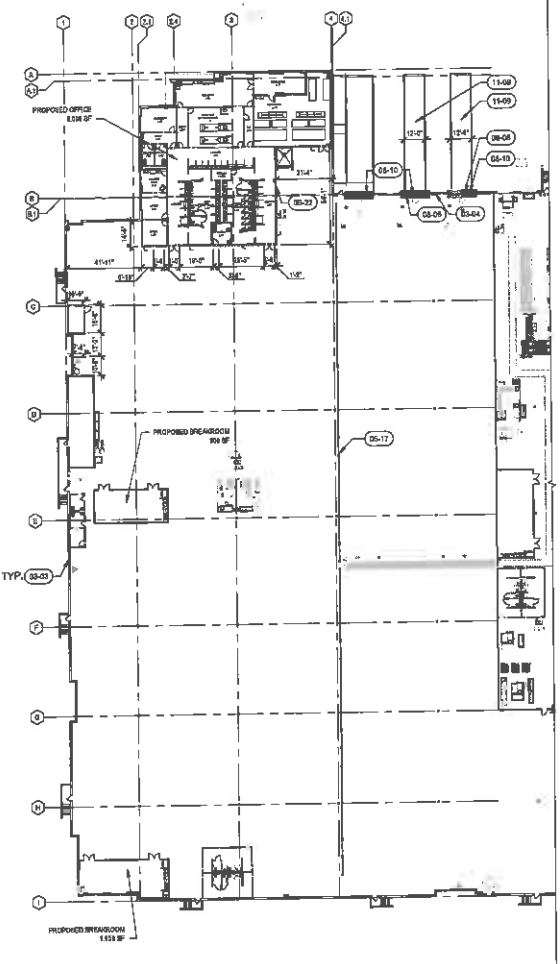


NO.	REVISION	DATE
1	ISSUE FOR PERMITS	07/20/11
2	ISSUE FOR CONSTRUCTION	07/20/11
3	ISSUE FOR CONSTRUCTION	07/20/11
4	ISSUE FOR CONSTRUCTION	07/20/11
5	ISSUE FOR CONSTRUCTION	07/20/11
6	ISSUE FOR CONSTRUCTION	07/20/11
7	ISSUE FOR CONSTRUCTION	07/20/11
8	ISSUE FOR CONSTRUCTION	07/20/11
9	ISSUE FOR CONSTRUCTION	07/20/11
10	ISSUE FOR CONSTRUCTION	07/20/11
11	ISSUE FOR CONSTRUCTION	07/20/11
12	ISSUE FOR CONSTRUCTION	07/20/11
13	ISSUE FOR CONSTRUCTION	07/20/11
14	ISSUE FOR CONSTRUCTION	07/20/11
15	ISSUE FOR CONSTRUCTION	07/20/11
16	ISSUE FOR CONSTRUCTION	07/20/11
17	ISSUE FOR CONSTRUCTION	07/20/11
18	ISSUE FOR CONSTRUCTION	07/20/11
19	ISSUE FOR CONSTRUCTION	07/20/11
20	ISSUE FOR CONSTRUCTION	07/20/11

PROJECT STATUS

OWNER	DESIGNER
DRAWN BY	CHECKED BY
DATE	DATE
SHEET TITLE	
PARTIAL FLOOR PLAN - OFFICE	
SHEET NUMBER	

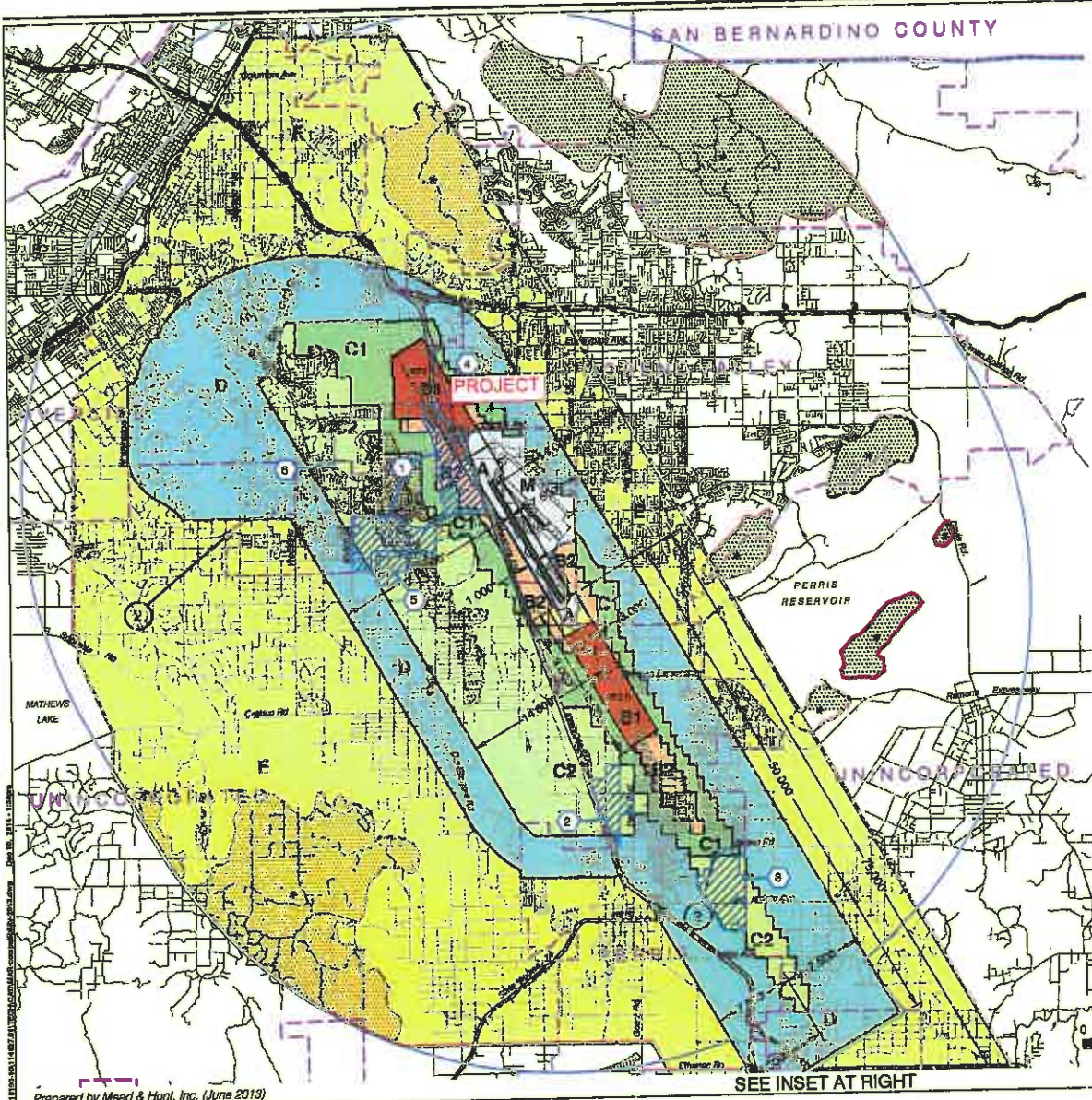
A1.14



1 PARTIAL FLOOR PLAN - OFFICE AREA
SCALE: 1/32"=1'-0"



DATE PLOTTED: 07/20/11



LEGEND

Compatibility Zones

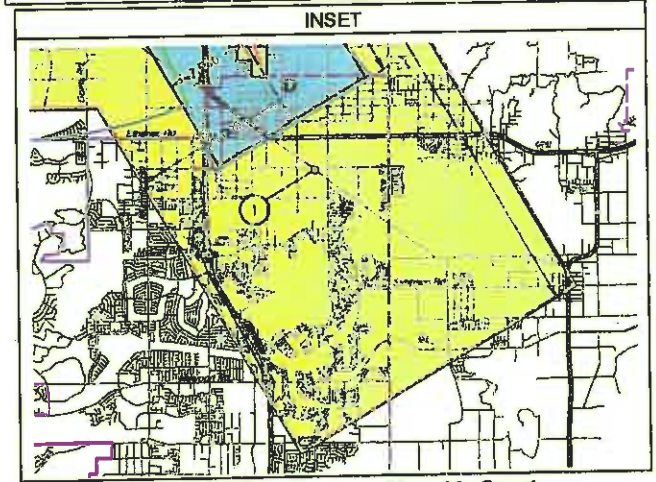
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

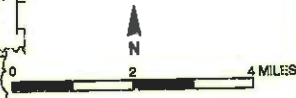
- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,835 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



**Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)**

Note:
All dimensions are measured from runway ends and centerlines.



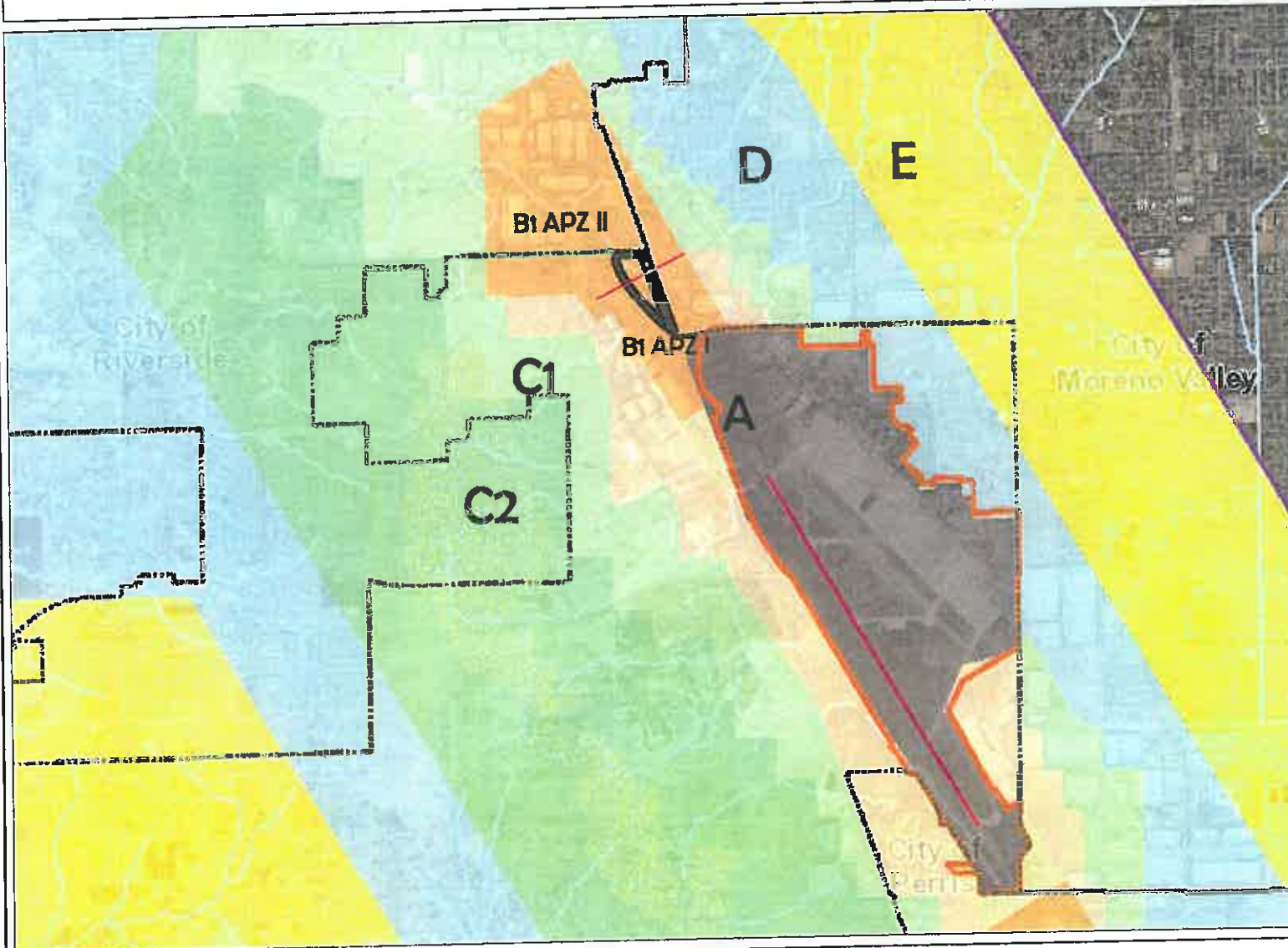
Base map source: County of Riverside 2013

Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map MA-1
Compatibility Map
March Air Reserve Base / Inland Port Airport

Map My County Map



Legend

- Runways
- ▣ Airports
- ▣ Airport Influence Areas
- Airport Compatibility Zones**
- ▣ OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6

Notes



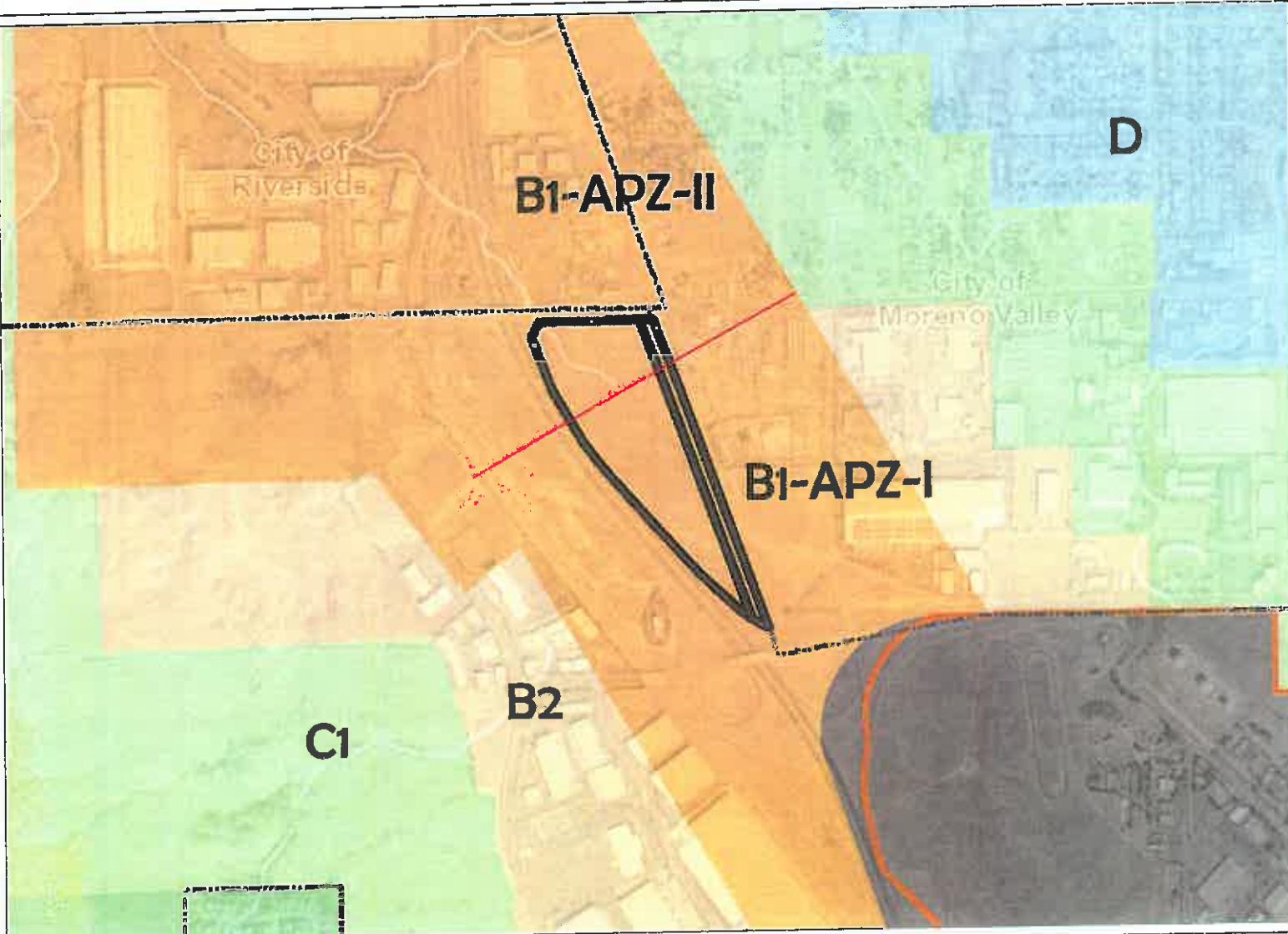
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.



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Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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Notes



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Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map

Notes



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0 1 3,032 Feet
516




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Map My County Map



Legend

-  Blueline Streams
-  City Areas
-  World Street Map

Notes



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0 6 12,127 Feet
064

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Map My County Map



- ### Legend
- Blueline Streams
 - City Areas
 - World Street Map



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Notes

Map My County Map



Legend

- Blue line Streams
- City Areas
- World Street Map

Notes



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0 1 3,032 Feet

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Legend

- Runway
- City of Moreno Valley
- City of Riverside
- APZ I
- APZ II
- CZ



1 inch = 0.36 miles

Rull, Paul

From: Housman, Simon
Sent: Tuesday, March 9, 2021 9:47 PM
To: Rull, Paul
Subject: Fwd: Freeway Business Center Amazon Delivery - ZAP1444MA20

Thank you.
S.A. Housman

Begin forwarded message:

From: "MCCRAINE, RODNEY E Col USAF AFRC 452 MSG/CC" <rodney.mccraine.3@us.af.mil>
Date: March 9, 2021 at 21:25:31 PST
To: "Housman, Simon" <shousman@rivco.org>
Cc: "UNDERWOOD, HAMILTON B III Lt Col USAF AFRC 452 MSG/CD" <hamilton.underwood@us.af.mil>, "SHAW, DAVID N Maj USAF AFRC 452 CE/CC" <david.shaw.5@us.af.mil>, "WATERS, DOUGLAS S GS-13 USAF AFRC 452 MSG/CE" <douglas.waters.2@us.af.mil>, "WELCH, SAMUEL T Maj USAF AFRC 452 AMW/JA" <samuel.welch.1@us.af.mil>
Subject: Freeway Business Center Amazon Delivery - ZAP1444MA20

Mr. Housman,

March ARB has reviewed the Airport Land Use Commission (ALUC) action; Freeway Business Center Amazon Delivery - ZAP1444MA20 (708k SF warehouse in APZs I and II, ALUC Zone B1). We find that the ALUC is recommending that a covenant be recorded on this property to restrict its use and occupancy to those established in Department of Defense Instruction 4165.57. However, because the enforcing government entity is March Joint Powers Authority (MJPA) we have questions over the enforceability of this covenant as well as two others that will be considered in April into the future and particularly if, and when, MJPA sunsets. We therefore ask for a continuance on the ALUC action until March ARB can determine what modifications, if any, should be made to the covenant to make it more enforceable.

Best Regards,

Rodney E. McCraine, Col, USAF
Commander, 452 Mission Support Group
March ARB, CA 92518
COMM 951-655-4221 (DSN 447)

Rull, Paul

From: WATERS, DOUGLAS S GS-13 USAF AFRC 452 MSG/CE <douglas.waters.2@us.af.mil>
Sent: Thursday, January 21, 2021 2:38 PM
To: Rull, Paul
Cc: Housman, Simon; MCCRAINE, RODNEY E Col USAF AFRC 452 MSG/CC; WELCH, SAMUEL T Maj USAF AFRC 452 AMW/JA; MARTIN, DAVID R
Subject: Fw: ZAP1444MA20 Freeway Business Center Amazon Delivery/Parcel Hub_Brian's Draft Comments
Attachments: Air Force MARB letter.pdf; March JPA_Amazon FBC_Rev Project Submittal.pdf; Amazon FBC Warehouse_Proposed Site Plan.pdf; ALUC_Proposed Amazon FBC Site Location.pdf

CAUTION: This email originated externally from the **Riverside County** email system.
DO NOT click links or open attachments unless you recognize the sender and know the content is safe

To meet your suspense for comments by COB Today, the following are March ARB comments as they concern the new proposed Amazon Parcel Distribution warehouse at JPA's Freeway Business Center.

Additional March ARB comments and a possible statement from leadership (Col Martin, Col McCraine, Col Morrison) could be given prior to or at the next public hearing on 11 February.

Doug Waters
Chief Engineering Flight
452 MSG/CEC
US Air Force Reserve Command
610 Meyer Dr., Bldg 2403
March ARB, CA 92518-2188

Douglas.waters.2@us.af.mil
Office- 951-655-4852
Cell- 928-304-4852
DSN- 447-2197

1. **MARB Air Force Letter from 10 March 2015 (attached for reference):** Since the revised proposed land use on this site (Parcel D3 East - Amazon distribution warehouse) is similar in scale to the previous proposed development (high-cube warehouse), MARB comments and concerns stated in attached AF letter remain valid as they concern:
 - A. Building height is not stated in plan review files received to date. Will require Airspace if greater than 35-foot height.
 - B. Water detention basins need to be oversized, covered, and/or underground to mitigate further rising groundwater table levels.
 - C. Stormwater management, drainage, and landscaping plans must adhere to BASH concerns.
 - D. Sound attenuation and hearing protection likely required for proposed office areas due to placement of building within the 65db AICUZ noise contour (CNEL).
 - E. As the proposed office space is contained entirely in APZ II, developer needs to clearly demonstrate compliance with AF and ALUC requirement of 50 persons per acre.

- F. Request ground elevation and building height to coincide with I. Obstruction Evaluation Service (OES) for potential finding of *Hazard to Air Navigation*, and to support MARB TERPS review.
 - G. Require that construction materials be of non-reflective material, as well as down-lighting to mitigate glint/glare levels which could undermine safe air operations during evening hours.
 - H. Proposed development shall not result in electronic and/or spectrum interference for aircraft communications or navigation capabilities.
2. **Office Density Covenant:** Both ALUC and March JPA mention a restrictive Covenant tied to allowable office densities as proposed in APZ II.
- A. Does said Covenant need to be revised per the new proposed Amazon distribtuon facility?
 - B. MARB requests a copy of the previous covenant to assist with further review of the proposed project.
 - C. Need March JPA to confirm the latest proposed number of full time occupants at 518.
 - D. Discrepancies noted between the latest March JPA project transmittal (dated 21 Dec. 2020) for total office area of 25,000 SF (22,000 SF on ground floor and 3,000 SF mezzanine). Received site plan proposes a total of 18,715 SF.
 - E. Total allowable office SF per AF/AICUZ and ALUC standards is 18,000 SF. Does the Covenant attempt to justify an additional 7,000 SF of office space in APZ II? Requires clarification.
3. **Traffic Impacts:** MARB is concerned about substantially increased traffic levels on both Cactus Avenue and Heacock Drive, as it is assumed the proposed Amazon parcel distribution facility will draw heavily from existing and proposed air cargo facility expansion (e.g. D-1 Gateway Aviation Center) at the March Inland Port.

V

From: WATERS, DOUGLAS S GS-13 USAF AFRC 452 MSG/CE <douglas.waters.2@us.af.mil>
Sent: Monday, January 18, 2021 10:58 AM
To: Rull, Paul <PRull@RIVCO.ORG>; Pacino, Brian <Brian.Pacino@jacobs.com>
Subject: [EXTERNAL] RE: ZAP1444MA20 Freeway Business Center Amazon Delivery/Parcel Hub

Yes we will have comments to you by Thursday.

Douglas Waters
Chief Engineering
951-655-4852

FY21 Year of March BCE

From: Rull, Paul <PRull@RIVCO.ORG>
Sent: Monday, January 18, 2021 8:03 AM
To: Pacino, Brian <Brian.Pacino@jacobs.com>; WATERS, DOUGLAS S GS-13 USAF AFRC 452 MSG/CE <douglas.waters.2@us.af.mil>
Subject: [Non-DoD Source] RE: ZAP1444MA20 Freeway Business Center Amazon Delivery/Parcel Hub
Importance: High

Good Morning,



DEPARTMENT OF THE AIR FORCE
AIR FORCE RESERVE COMMAND

10 March 2015

MEMORANDUM FOR RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
ATTN: JOHN GUERIN
PRINCIPAL PLANNER
4080 LEMON STREET, 14TH FLOOR
RIVERSIDE, CA 92501

FROM: 452d Mission Support Group/Civil Engineers
Base Operating Support
610 Meyer Drive, Bldg. 2403
March ARB CA 92518-2166

SUBJECT: Riverside County Airport Land Use Commission (RCALUC) – ZAP1107MA14

1. The March Air Reserve Base (MARB) review of the proposal to establish Industrial zoning on 39.42 acres and to build a 709,083 square foot industrial warehouse building is provided with this memorandum.
2. The parcel also known as D3 East is located within the Accident Potential Zone I (APZ I) and Accident Potential Zone II (APZII). Any construction in APZ I is to consist of facilities that are no greater than single floor, airspace review is required for objects greater than 35ft. in height. Lot coverage is based on calculation in the Floor Area Ratios (FAR). Only a few types of facilities are compatible in this zone. There are restrictions on land uses and heights of natural objects and man-made objects in the vicinity of air installations that may obstruct the airspace, attract birds, cause electromagnetic or thermal interference, or produce dust, steam, smoke, or light emissions to provide for safety of flight and the public welfare.
3. The parcel is partially located within the Perris North sub-basin, the same groundwater sub-basin as MARB. The rising groundwater table at MARB is an ongoing concern and solutions are being sought by both MARB and the State of California. Given the concerns with the rising groundwater and the ability to properly drain the water detention basins within 48 hours, MARB is requesting the water detention basins be oversized enough to accept additional rock to address future concerns with groundwater or be underground and covered. Prior to issuance of formal approval, we want to see specific design of basins and if uncovered, calculations that show capacity if rock is added later.
4. A properly designed stormwater management system and landscaping must address Bird/Wildlife Aircraft Strike Hazard (BASH) concerns including proper detention/infiltration of stormwater runoff. The base will want to review details of the stormwater conveyance system and the landscaping plan when they become available. Given the proximity to the airfield, trees which will bear mast or grow to an adequate size for roosting should not be planted. Additional information on reducing BASH hazards can be found in AFPAM 91-212, *Bird/Wildlife Aircraft Strike Hazard (BASH) Management Techniques*, dated February 1, 2004. We request that March Joint Powers Authority evaluate the stormwater detention basin design to mitigate or eliminate any hazards, and jointly approve the design with MARB.
5. The property is impacted by aircraft noise with California Noise Equivalency Levels and a Day-Night Average A-Weighted Sound Level (DNL) of 65 decibels or more contributing to negatively impact

compatible and reasonable use of the property. Table 2, *Land Use Compatibility in Noise Zones* in Appendix 3 to Enclosure 3, *Recommended Land Use Compatibility in Noise Zones* of DoDI 4165.57, *Air Installations Compatible Use Zones* (AICUZ) states that noise events may be sufficient to periodically disrupt indoor activities. Employees and regularly received public may require protection using noise attenuation in the design and construction of the facility. Additional hearing protection for employees may be required by OSHA or other agencies as it relates to safety and health in a high noise level work environment.

6. While the proposed use may be consistent with the zoning and land use guidelines, MARB advises that the proposed project presents a concern being located so close to the Clear Zone (CZ) at the north end of the runway. Buildings in this area should not be used for high-density functions since the objective of the land use guidelines in and around APZ's is to restrict people-intensive use due to a greater risk of aircraft incident in these areas. In APZ I is restricted to 25 people per acre and 50 an acre in APZ II. Lot coverage is based on the FAR, and is calculated using standard parking generation rates for various land uses, vehicle occupancy rates, and desired density in APZ I and II. For APZ I, the formula is $FAR = 25 \text{ people an acre} / (\text{Average Vehicle Occupancy} \times \text{Average Parking Rate} \times (43560/1000))$. The formula for APZ II is $FAR = 50 / (\text{Average Vehicle Occupancy} \times \text{Average Parking Rate} \times (43560/1000))$.

7. The building height is a cause for concern. We request the latitude and longitude of the southeast and northeast corners and the ground elevation of the warehouse along this façade in order to confirm the building falls within established height restrictions. Consultation with the Federal Aviation Administration will be required and we will also need to provide a Terminal Instrument Procedures (TERPS) review.

8. To help eliminate any potential effects on aircraft operations at MARB, we ask that materials provided in construction be of a non-reflective material such as outside ductwork, windows and roofs by means such as painting or covering. In addition, none of the project improvements shall create:

- Distracting lights which could be mistaken for airport lights
- Sources of dust, steam, or smoke which may impair pilot visibility
- Sources of electronic interference with aircraft communications or navigation

9. Thank you for the opportunity to again, review and comment on this proposed development. If you have questions please contact Ms. Denise Hauser at (951) 655-4862, or Sonia Pierce at (951) 655-2236.


PAMELA M. HANN
Base Civil Engineer

Scenario	Occupancy Ratio	Building Area (sq.ft.)	Total Occ.	Average Acre (ppl per acre)	Single Acre (ppl)	Consistent w ALUC	Consistent w AICUZ
<p>[ALUC Standard Method] Utilizing the building code calculations methodology used in previously approved project ZAP1394MA19, and applying it to the proposed plan</p>	High Cube Ware. 1/1,428 Office 1/200 Break Room 1/15	699,583 8,000 1,500	630	APZ-I 12 APZ-II 21	APZ-I 31 APZ-II 130	Yes No exceed single acre, break room + HCH warehouse	No. Yes, with the use of a Covenant
<p>[Hybrid Methodology] Building code calculating floor area usage, and parking code calculating the indoor van parking and stacking spaces. Parking code calculation is reduced to 1 person per space (vs standard 1.5 person per space) Doesn't account for floor area dead space. (previous staff report calculation)</p>	High Cube Ware. 1/1,428 Office 1/200 Break Room 1/15 Van parking space 1 Van loading space 1	258,000 8,000 1,500 70 126	518	APZ-I 10 APZ-II 17	APZ-I 52 APZ-II 100	Yes Yes	No. Yes, with the use of a Covenant
<p>[Hybrid Methodology 1: Unidentified Floor Area] Building code calculating floor area usage, and parking code calculating the indoor van parking and stacking spaces. Parking code calculation is reduced to 1 person per space (vs standard 1.5 person per space) Building code requires all floor area to be identified. Therefore, the primary use of high-cube warehouse is applied to floor areas that are not identified.</p>	High Cube Ware. 1/1,428 Dead space as high cube Office 1/200 Break Room 1/15 Van parking space 1 Van loading space 1	258,000 120,600 8,000 1,500 70 126	603	APZ-I 12 APZ-II 19	APZ-I 52 APZ-II 120	Yes No exceed single acre, break room + dead space HCH warehouse	No. Yes, with the use of a Covenant
<p>[Building Code Strict] Building code does not have an occupancy calculation for high-cube warehouse, indoor van parking/stacking spaces. Therefore, a regular 'warehouse' ratio is applied per County Building & Safety. Building code also requires all floor area to be identified. Therefore, the primary use of warehouse is applied to areas that are not identified.</p>	Warehouse 1/500 Dead space as warehouse Office 1/200 Break Room 1/15	699,583 8,000 1,500	1,523	APZ-I 35 APZ-II 39	APZ-I 87 APZ-II 158	No, exceed average acre No, exceed single acre, break room + dead space warehouse	No. Yes, with the use of a Covenant

Scenario	Occupancy Ratio	Building Area (sq.ft.)	Total Occ.	Average Acre (ppl per acre)	Single Acre (ppl)	Consistent w ALUC	Consistent w AICUZ
<p>[Hybrid Methodology 2: All Floor Area Identified] Building code calculating floor area usage, and parking code calculating the indoor van parking and stacking spaces. Parking code calculation is reduced to 1 person per space (vs standard 1.5 person per space)</p> <p>Building code requires all floor area to be identified. The applicant has identified all previous unidentified floor areas on their floor plan</p>	High Cube Ware. 1/1,428 Office 1/200 Break Room 1/15 Van parking space 1 Van loading space 1	307,000 8,000 5,400 164 126	905	APZ-I 13 APZ-II 40	APZ-I 56 APZ-II 377	Yes No exceed single acre, break room + dead space HCH warehouse	No, Yes, with the use of a Covenant

Occupancy Survey for 2677 Alessandro Boulevard in Riverside County, CA

Site Surveyed: 9350 Rayo Avenue, South Gate, CA 90280

The Riverside County Airport Land Use Commission’s (ALUC) Airport Land Use Plan permits the survey of similar uses as an acceptable method to determine concentrations of people for occupancy purposes. The proposed tenant has obtained a survey of a comparable use located at 9350 Rayo Avenue in South Gate, CA (the “comparable facility”) and has attached such data. When Rayo’s surveyed rate of 1 person per 70 SF is applied to the proposed 6,500 SF breakroom area of 2677 Alessandro Blvd., it would equate to a headcount of 93 people.

9350 Rayo Ave. is a logical analog facility for Alessandro: it is a parcel distribution station. The facility’s operations are functionally equivalent with a similar capacity to the proposed quantities at 2677 Alessandro Blvd. At approximately 211,356 SF, the comparable facility is sized smaller than Alessandro; however, Alessandro’s larger size relative to its operations is a direct function of efficiency as Rayo is significantly more efficient relative to its building square footage. The efficiency can be evidenced by the warehouse operational area within Alessandro, which is less than 37% of its building footprint.

Specifically, the data from 9350 Rayo Ave. was collected from the log of electronic security card-in points located adjacent to its 4,720 SF breakroom. The log includes “access granted” activity from February 1st – March 2nd, 2021 (30 days). The average count during the survey period averages to about 67 persons per daily period or approximately one person per 70 SF of breakroom area. The two buildings' operational capacity is expected to be roughly equivalent, and this is the most significant factor in their similarity for being comparable. When the one person per 70 SF surveyed occupancy rate is applied to the proposed 6,500 SF breakroom, it amounts to a 93-person headcount for occupancy calculation purposes. This is roughly half-way between an office use occupancy rate (1 person per 200SF) and the overly concentrated assembly use occupancy rate (1 person per 15 SF) that may be projected in the building code methodology and conservatively skews toward the latter. The surveyed data is a more realistic predictor for occupancy purposes, given that it represents real-life information as a proxy.

Given the foregoing, the data strongly suggests that applying the survey method to the proposed 6,500 SF Alessandro breakroom area would be a defensible and equitable method for occupancy purposes. Therefore, it is requested that an occupancy rate of 1 person 70 SF apply to the Alessandro breakroom.

Summary of Daily Counts: February 1st - March 2nd							
1-Feb	97	9-Feb	65	17-Feb	67	25-Feb	69
2-Feb	68	10-Feb	67	18-Feb	69	26-Feb	70
3-Feb	73	11-Feb	76	19-Feb	66	27-Feb	68
4-Feb	62	12-Feb	74	20-Feb	60	28-Feb	59
5-Feb	51	13-Feb	71	21-Feb	69	1-Mar	61
6-Feb	64	14-Feb	78	22-Feb	66	2-Mar	47
7-Feb	78	15-Feb	63	23-Feb	55		
8-Feb	74	16-Feb	60	24-Feb	73		

9350 Rayo Avenue, South Gate, CA 90280 Data

<u>System</u>	<u>Categories</u>	<u>Date</u>	<u>Hour Starting</u>	<u>Hourly Headcount</u>
OnGuard 7.4	Access Granted by Reader	2/1/2021	1:00am	45.00
OnGuard 7.4	Access Granted by Reader	2/1/2021	2:00am	-
OnGuard 7.4	Access Granted by Reader	2/1/2021	3:00am	-
OnGuard 7.4	Access Granted by Reader	2/1/2021	4:00am	3.00
OnGuard 7.4	Access Granted by Reader	2/1/2021	5:00am	7.00
OnGuard 7.4	Access Granted by Reader	2/1/2021	6:00am	1.00
OnGuard 7.4	Access Granted by Reader	2/1/2021	7:00am	2.00
OnGuard 7.4	Access Granted by Reader	2/1/2021	8:00am	17.00
OnGuard 7.4	Access Granted by Reader	2/1/2021	9:00am	1.00
OnGuard 7.4	Access Granted by Reader	2/1/2021	10:00am	-
OnGuard 7.4	Access Granted by Reader	2/1/2021	11:00am	-
OnGuard 7.4	Access Granted by Reader	2/1/2021	12:00am	2.00
OnGuard 7.4	Access Granted by Reader	2/1/2021	1:00pm	1.00
OnGuard 7.4	Access Granted by Reader	2/1/2021	2:00pm	-
OnGuard 7.4	Access Granted by Reader	2/1/2021	3:00pm	4.00
OnGuard 7.4	Access Granted by Reader	2/1/2021	4:00pm	4.00
OnGuard 7.4	Access Granted by Reader	2/1/2021	5:00pm	1.00
OnGuard 7.4	Access Granted by Reader	2/1/2021	6:00pm	-
OnGuard 7.4	Access Granted by Reader	2/1/2021	7:00pm	1.00
OnGuard 7.4	Access Granted by Reader	2/1/2021	8:00pm	-
OnGuard 7.4	Access Granted by Reader	2/1/2021	9:00pm	-
OnGuard 7.4	Access Granted by Reader	2/1/2021	10:00pm	1.00
OnGuard 7.4	Access Granted by Reader	2/1/2021	11:00pm	1.00
OnGuard 7.4	Access Granted by Reader	2/1/2021	12:00pm	6.00
				<hr/>
				97.00

Note: This below data requested from a specific facility reflects "Access Granted" events by personnel. Each has a corresponding employee ID#, which has been modified to a "blinded" ID# due to data privacy concerns.

"Access Granted" Counts from 20-day Survey at Range Facility

Counts relative to the 4,720 SF breakroom an unattended facility calculates to 1 person per 1.18 sq ft. Breakroom based on a proposed 6,200 SF breakroom 53 person headcount

Summary of Daily Counts February 20 - March 2020. Table with columns: Date, Count, and Daily Count. Shows daily access counts from Feb 20 to Feb 28, with a total of 97 counts.

Main table of Access Granted events. Columns: Access Granted By, Access Granted By Reader, Query, Start Date, End Date, Reader ID, and Reader Name. Lists thousands of individual access events.

Report Generated Dates table. Columns: Report Date, Access Granted By, Access Granted By Reader, Query, Start Date, End Date, Reader ID, and Reader Name. Shows a chronological list of report dates and associated access events.

OnGuard 7.A Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/1/2021 11:59:59 PM; READERS: DAK7-1.20-PCIP UP RM IN, DAK7-1.20-4BR-ROOM ENT

Report Date: 2/1/2021 1:57:42PM Eastern Standard Time Event Details 2/1/2021 Access Granted 34192390 57

OnGuard 7.A Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/1/2021 11:59:59 PM; READERS: DAK7-1.20-PCIP UP RM IN, DAK7-1.20-4BR-ROOM ENT

Report Date: 2/1/2021 1:57:42PM Eastern Standard Time Event Details 2/1/2021 Access Granted 34192390 58

OnGuard 7.A Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/1/2021 11:59:59 PM; READERS: DAK7-1.20-PCIP UP RM IN, DAK7-1.20-4BR-ROOM ENT

Report Date: 2/1/2021 1:57:42PM Eastern Standard Time Event Details 2/1/2021 Access Granted 34192390 59

OnGuard 7.A Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/1/2021 11:59:59 PM; READERS: DAK7-1.20-PCIP UP RM IN, DAK7-1.20-4BR-ROOM ENT

Report Date: 2/1/2021 1:57:42PM Eastern Standard Time Event Details 2/1/2021 Access Granted 34192390 60

OnGuard 7.A Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/1/2021 11:59:59 PM; READERS: DAK7-1.20-PCIP UP RM IN, DAK7-1.20-4BR-ROOM ENT

Report Date: 2/1/2021 1:57:42PM Eastern Standard Time Event Details 2/1/2021 Access Granted 34192390 61

OnGuard 7.A Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/1/2021 11:59:59 PM; READERS: DAK7-1.20-PCIP UP RM IN, DAK7-1.20-4BR-ROOM ENT

Report Date: 2/1/2021 1:57:42PM Eastern Standard Time Event Details 2/1/2021 Access Granted 34192390 62

OnGuard 7.A Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/1/2021 11:59:59 PM; READERS: DAK7-1.20-PCIP UP RM IN, DAK7-1.20-4BR-ROOM ENT

Report Date: 2/1/2021 1:57:42PM Eastern Standard Time Event Details 2/1/2021 Access Granted 34192390 63

OnGuard 7.A Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/1/2021 11:59:59 PM; READERS: DAK7-1.20-PCIP UP RM IN, DAK7-1.20-4BR-ROOM ENT

Report Date: 2/1/2021 1:57:42PM Eastern Standard Time Event Details 2/1/2021 Access Granted 34192390 64

OnGuard 7.A Access Granted by Reader QUERY: START DATE: 3/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DA07-1.20-03-PCUP RM IN, DA07-1.20-04-BEARROOM ENT

Table with columns: Report Date, Report Description, Event Details, and Access Granted/Failed status. Includes rows for various dates from 3/1/2021 to 3/2/2021, detailing system standard times and reader events.

Table with columns: Request ID, Status, Action, Requester, Start Date, End Date, Requested By, and Description. The table lists numerous requests for room access, primarily for the 'BEARINGS' room, with dates ranging from 2/1/2021 to 2/22/2021.

Table with columns: Request ID, Status, Action, Requester, Start Date, End Date, Requested By, and Description. This table continues the list of requests, including various room access requests and standard time requests, with dates ranging from 2/1/2021 to 2/22/2021.

Table with columns: Request ID, Status, Action, Requester, Start Date, End Date, Requested By, and Description. This table contains the final entries of requests, including room access requests, with dates ranging from 2/1/2021 to 2/22/2021.

Outbound 7.4 Access Granted by Reader QUERY: START DATE: 3/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:59:59 PM; READERS: DAW7-1.20-PC-FPK UP RM IN, DAW7-1.20-REARROOM ENT

Table with columns: Report Date, Report Time, Report Type, Event Details, Access Granted, and Access Denied. Contains a list of access events for various dates in 2021, including dates like 3/1/2021, 3/2/2021, and 3/3/2021.

Table with columns: OnGuard 7.4, Access Granted by Reader, QUERY: START DATE, END DATE, READERS. Lists access events for various readers and dates.

Table with columns: Report Date, Event Details, Access Granted, Access Denied, Access Granted. Lists report dates and corresponding access event counts.



2677 E ALESSANDRO BLVD
RIVERSIDE, CA 92508

TENANT IMPROVEMENT
FREEWAY BUSINESS CENTER
2677 E ALESSANDRO BLVD
RIVERSIDE, CA 92508

DESCRIPTION	A	DATE
ALICE PO Number		12/2/2021

PROJECT STATUS

JOB NUMBER 20-8215-01

DRAWN BY BSJ CHECKED BY JD

SHEET TITLE EXTERIOR ELEVATIONS

SHEET NUMBER

A2.10

GENERAL NOTES

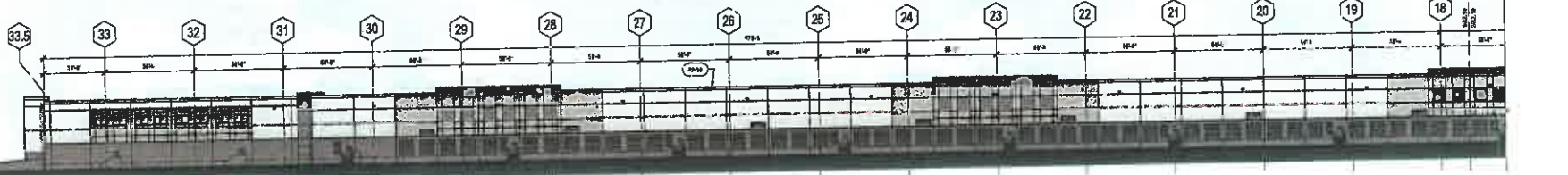
- ELEVATION 0'-0" = FINISH FLOOR ELEVATION.
- ALL VERTICAL DIMENSIONS ARE FROM FINISHED FLOOR, UNLESS NOTED OTHERWISE.
- CHANGE IN PAINT COLOR OCCURS AT BACK OF REVEAL, TYP, UNLESS NOTED OTHERWISE.

KEYNOTES

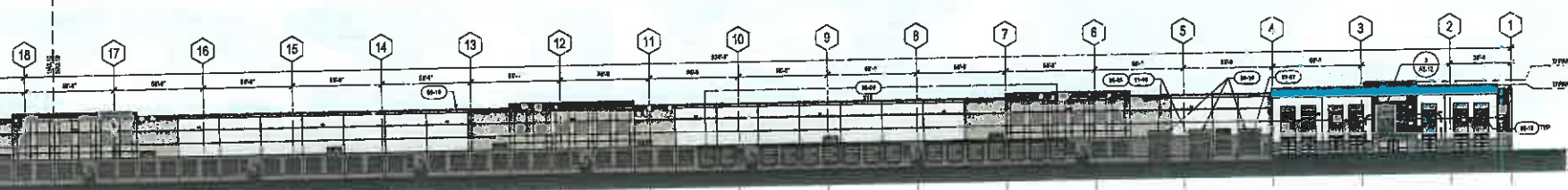
- 08-12: METAL PANEL FABRIC - FIELD PAINT - REFER TO COLOR LEGEND
- 25-14: NEW CORRUGATED METAL ENTRY CA WOPY.
- 28-05: NEW OVERHEAD 12 X 14' DRIVE-IN DOOR PAINT TO MATCH EXISTING EQUIPMENT PER TENANT DESIGN GUIDELINES.
- 28-06: NEW INSULATED OVERHEAD DOCK DOOR, PROVIDE NEW DOCK EQUIPMENT PER TENANT DESIGN GUIDELINES.
- 28-07: EXISTING 12 X 14' DRIVE-IN DOOR.
- 08-12: HIGH SPEED FABRIC ROLL UP DOOR (EXTERIOR MOUNT)
- 09-10: INSTALL PRIVACY FILM ON EXISTING GYRON FRONT SECTIONS SHOWN.
- 09-10: NEW PARTIALLY ACCENT STRIPS, ALIGN WITH EXISTING REVEALS
- 15-24: NEW INTERNALLY ILLUMINATED BUILDING MOUNTED TENANT SIGNAGE.
- 11-09: NEW PREFABRICATED 12' WIDE DRIVE OUT RAMP.

EXTERIOR PAINT COLOR LEGEND

MARK	DESCRIPTION	REMARKS
15-10	PAINT / TEXTURED ACRYLIC COATING (CONCRETE WALL PANELS)	
	PRIME BLUE TO MATCH PANTONE 2985C	NEW EXTERIOR ACCENT COLOR
	EXISTING EXTERIOR COLOR TO REMAIN: GLODEN PAINTS A1924 WINTER 8182	EXISTING FIELD COLOR COLOR
	EXISTING EXTERIOR COLOR TO REMAIN: GLODEN PAINTS A1935 ZEPHYRUS	EXISTING ACCENT COLOR
	EXISTING EXTERIOR COLOR TO REMAIN: GLODEN PAINTS A1932 MANSARD STONE	EXISTING ACCENT COLOR



1 PARTIAL EAST ELEVATION
SCALE: 1/32" = 1'-0"



2 PARTIAL EAST ELEVATION
SCALE: 1/32" = 1'-0"

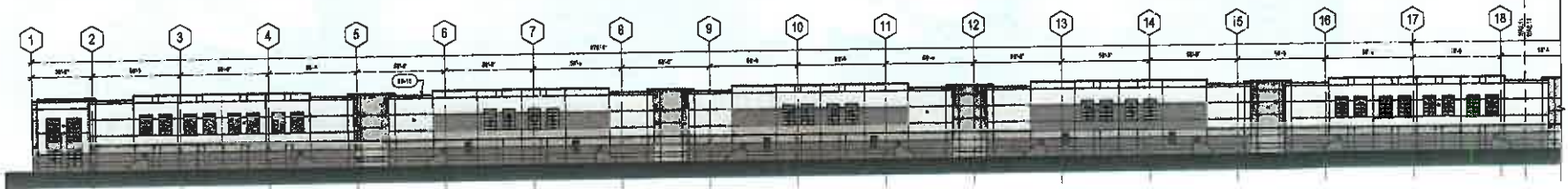
PL07 EXHIBIT 31 DWGDATE 20210915



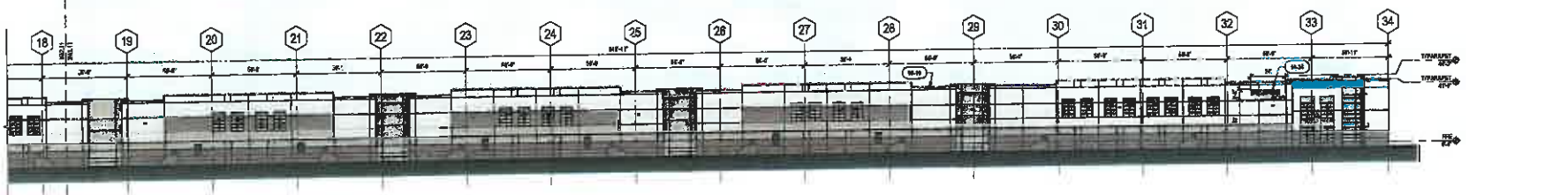
2677 E ALESSANDRO BLVD
RIVERSIDE, CA 92505

**TENANT IMPROVEMENT
FREEWAY BUSINESS CENTER**
2677 E ALESSANDRO BLVD
RIVERSIDE, CA 92508

GENERAL NOTES		KEYNOTES
<p>A. ELEVATION 9'-0" = FINISH FLOOR ELEVATION. B. ALL VERTICAL DIMENSIONS ARE FROM FINISHED FLOOR UNLESS NOTED OTHERWISE. C. CHANGE IN PAINT COLOR OCCURS AT BACK OF REVEAL, TYP, UNLESS NOTED OTHERWISE.</p>		<p>05-12 METAL PANEL FASCIA - FIELD PAINT - REFER TO COLOR LEGEND 05-14 NEW CORRUGATED METAL ENTRY CA HOPT. 05-02 NEW OVERHEAD 12 X 14 DRIVE-IN DOOR PAINT TO MATCH EXISTING 08-08 NEW INSULATED OVERHEAD DOCK DOORS, PROVIDE NEW DOCK EQUIPMENT PER TENANT DESIGN (0.8 #1) LINE. 09-07 EXISTING 12 X 14 DRIVE-IN DOOR. 05-10 HIGH SPEED FABRIC ROLL UP DOOR (EXTERIOR MOUNT) 08-10 NEW PAINTED ACCENT STRIPE, ALIGN WITH EXISTING REVEALS 10-34 NEW INTERNALLY ILLUMINATED BLUE LING MOUNTED TENANT SIGNAGE. 11-09 NEW PREFABRICATED 12 WIDE DRIVE CUT RAMP.</p>
EXTERIOR PAINT COLOR LEGEND		
MARK	DESCRIPTION	REMARKS
	PRIME BLUE, TO MATCH PANTONS 2966C	NEW EXTERIOR ACCENT COLOR
	EXISTING EXTERIOR COLOR TO REMAIN GLEDDEN PAINTS A1453 MENTOR W/HD	EXISTING FIELD COLOR
	EXISTING EXTERIOR COLOR TO REMAIN GLEDDEN PAINTS A1453 ZEPPELIN	EXISTING ACCENT COLOR
	EXISTING EXTERIOR COLOR TO REMAIN GLEDDEN PAINTS A1522 MANHATTAN STONE	EXISTING ACCENT COLOR



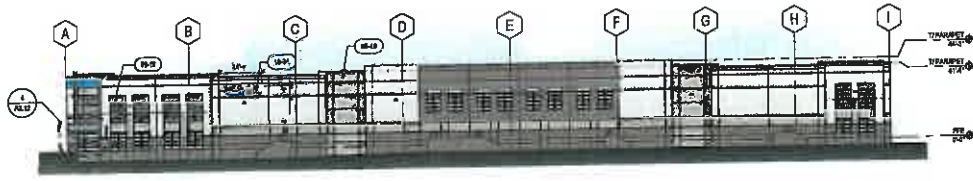
1 PARTIAL WEST ELEVATION
SCALE: 1/32" = 1'-0"



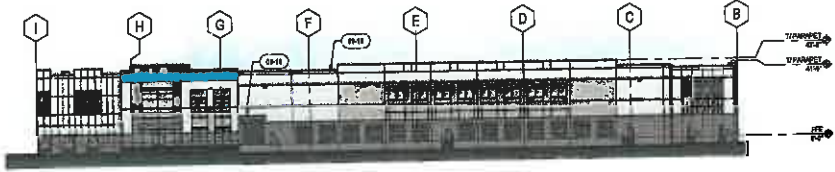
2 PARTIAL WEST ELEVATION
SCALE: 1/32" = 1'-0"

DATE	DESCRIPTION	BY	CHKD BY

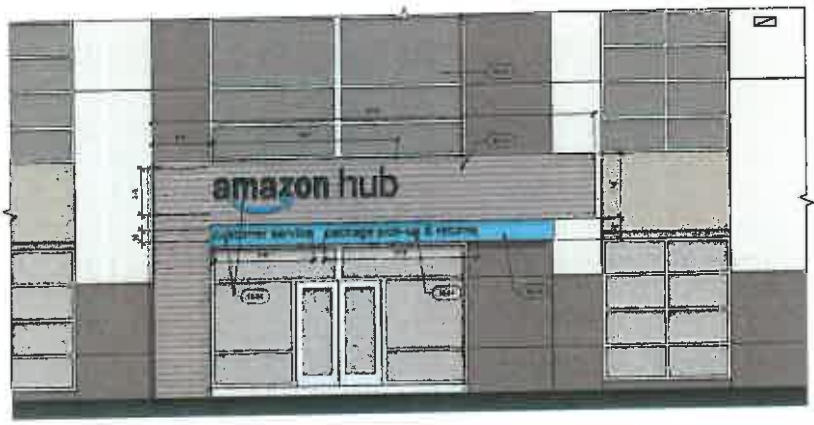
PROJECT STATUS	
JOB NUMBER	20-5910-01
DRAWN BY	BSJ
CHECKED BY	JC
SHEET TITLE	EXTERIOR ELEVATIONS
SHEET NUMBER	A2.11



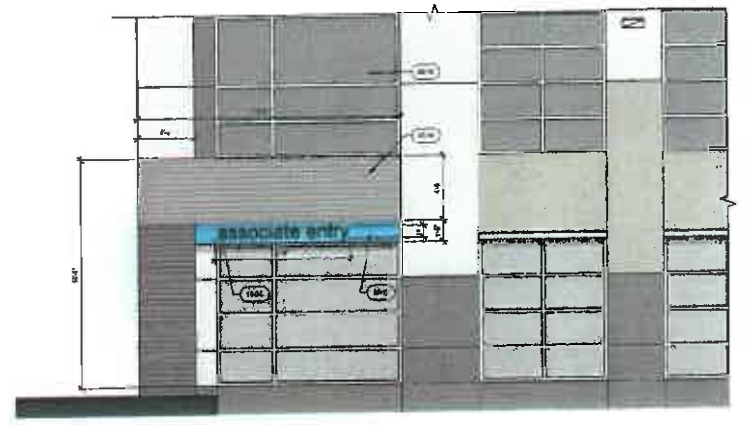
1 NORTH ELEVATION
SCALE: 1/32" = 1'-0"



2 SOUTH ELEVATION
SCALE: 1/32" = 1'-0"



3 EAST ENLARGED ELEVATION
SCALE: 1/4" = 1'-0"



4 NORTH ENLARGED ELEVATION
SCALE: 1/4" = 1'-0"

GENERAL NOTES

- A. ELEVATION 6'-0" = FINISH FLOOR ELEVATION
- B. ALL VERTICAL DIMENSIONS ARE FROM FINISHED FLOOR, UNLESS NOTED OTHERWISE.
- C. CHANGE IN PAINT COLOR OCCURS AT LACK OF REVEAL, TYP, UNLESS NOTED OTHERWISE.

EXTERIOR PAINT COLOR LEGEND

MARK	DESCRIPTION	REMARKS
EXTERIOR PAINT / TEXTURED ACRYLIC COAT/MS (CONCRETE WALL PANELS)	PRIME BLUE, TO MATCH PAINT/MS 29997	NEW EXTERIOR ACCENT COLOR
	GLIDDEN PAINTS A1653 WILD BIRD	EXISTING FIELD COLOR TO REMAIN
	GLIDDEN PAINTS A1653 ZEPHYRUS	EXISTING ACCENT COLOR TO REMAIN
	GLIDDEN PAINTS A1182 MANSARD STONE	EXISTING ACCENT COLOR TO REMAIN

KEYNOTES

- 05-12 METAL PANEL FRAME - FIELD PAINT - REFER TO COLOR LEGEND
- 07-04 NEW CORRUGATED METAL ENTRY CANOPY
- 08-06 NEW OVERHEAD 12 X 14 DRIVE-IN DOOR. PAINT TO MATCH EXISTING
- 08-16 HIGH SPEED FABRIC ROLL UP DOOR (EXTERIOR MOUNT)
- 08-16 INSTALL PRIVACY FILM ON EXISTING 6'0" REFRIG. SECTIONS SHOWN
- 08-16 NEW PAINTED ACCENT STRIPE, ALIGN WITH EXISTING REVEALS
- 19-04 BUILDING MOUNTED TENANT STORAGE



2877 E ALESSANDRO BLVD
RIVERSIDE, CA 92508

**TENANT IMPROVEMENT
FREIGHTWAY BUSINESS CENTER**
 2877 E ALESSANDRO BLVD
 RIVERSIDE, CA 92508

DESCRIPTION	DATE	SCALE

PROJECT STATUS

JOB NUMBER: 20-09-10-01
 DRAWN BY: BGI CHECKED BY: JD
 SHEET TITLE: EXTERIOR ELEVATIONS

SHEET NUMBER:
A2.12

AMAZON LOGISTICS OPERATIONAL NARRATIVE

Operational Overview

Amazon Logistics (“AMZL”) is a service that fulfills customer orders. AMZL specializes in “last mile” delivery of customer orders from delivery stations. Packages are shipped to AMZL delivery stations from Amazon fulfillment and sortation centers. Packages arrive from line haul trucks, are sorted based on zip codes and loaded into delivery vans operated by delivery service partners (“DSP”).

Delivery stations operate 24/7, with most of the sortation activity done early in the morning when the line haul trucks arrive with customer packages. At our proposed Riverside County, California facility, AMZL line haul trucks will be delivering packages to the delivery station each day, primarily between the hours of 10:00 PM to 8:00 AM. Associates sort the packages by routes, place the packages onto movable racks and load the packages into the delivery vans primarily between 12:30 AM and 11:00 AM with Amazon associates entering and departing between those times. Additionally, there will be managers supervising the DSP operations, arriving between 8:00 AM and 11:00 AM and departing between 7:00 PM and 10:00 PM.

The first “wave” of DSP drivers arrive at the delivery station at approximately 10:00 AM. At this location, DSP drivers will typically park their personal vehicles onsite and pick up their delivery vans. To keep site operations efficient and secure, DSP drivers are instructed to arrive only just before their scheduled shift time. Once at the delivery station within their delivery van, DSP drivers take position in a vehicle queue that spaces out the group and secures their movement for optimal organization. In a synchronized manner, the drivers load their individual delivery vans within sequenced, segmented groups and depart to deliver packages directly to customers. Each delivery wave takes about 20-30 minutes to load and depart. During this time, the van positions in the facility of earlier scheduled driver groups provide a physical buffer to the entry of later arriving groups. This standard operating procedure acts as an added measure to ensure proper occupancy of the building at this location. As a wave of DSP drivers prepare to depart, a new wave of DSP drivers queue and prepare to load their delivery van. The last wave of DSP drivers depart the delivery station around 1:00 PM. DSP van drivers typically complete one round trip per day.

After DSP drivers complete their routes, they return to the delivery station with any packages that may have been non-deliverable. They typically return to the facility between 7:00 PM to 10:00 PM or after rush hour traffic. After proper checkout and release, the DSP drivers typically park their delivery vans onsite, and leave using a personal vehicle or public transport.

After departure of the last wave of delivery vehicles, delivery station associates prepare the delivery station for the next day’s packages.

Employment

Amazon strives to provide job local opportunities for the communities and the residents that it serves. Historically, Amazon has conducted local hiring fairs and has partnered with local workforce development organizations to market new Amazon employment opportunities to residents who live in and around its facilities. We have found that building local partnerships is key to a successful local hiring initiative. Our growth is the result of an outstanding workforce, strong local support, and incredible customers. Our

associates and customers in this region are also your residents, and we want to ensure we are being good neighbors.

Delivery Stations create hundreds of mostly full-time and part-time jobs. Amazon provides all associates starting at least \$15 per hour in wages, and a variety of benefits packages from day one. Wages in the California are currently trending significantly higher than \$15. All full time employees receive benefits including medical, dental and vision insurance, 401k match, and life and disability insurance. Part-time opportunities are great for parents seeking flexibility in schedule, college students, those seeking second jobs, and retirees. For the proposed delivery station, we are planning 70% full-time jobs.

Four types of jobs are available in delivery stations:

1. Sortation – These associates are directly employed by Amazon. They help with sorting packages inside the delivery station.
2. Delivery Service Partners (DSP) – DSPs are entrepreneurs who have launched their own small business delivering packages on behalf of Amazon. DSPs operate out of Amazon’s delivery stations and employ delivery van drivers who deliver Amazon packages. They adhere to the minimum \$15 per hour wage requirement for their employees. After 30 days of hire, DSPs are eligible for benefits.
3. Managers – Managers are employed by Amazon for managing the sortation process, and by DSP owners for managing the delivery process.
4. Flex Drivers – Flex drivers are independent contractors who own their own vehicles and create their own schedules delivering packages on behalf of Amazon. Amazon Flex drivers can earn more than between \$18 and \$22 per hour. However, these positions do not apply at this facility.

Environmental Plan

In alignment with the County’s policies to promote healthy and sustainable communities, Amazon is committed to being a leader in sustainability. In support of The Climate Pledge commitment to be net zero carbon by 2040, 10,000 Amazon custom electric delivery vehicles will be on the road delivering to customers worldwide as early as 2022 and all 100,000 by 2030. Amazon has been busy engaging several small startups and vehicle manufacturers to pursue multiple simultaneous paths.

As part of Amazon’s commitment to power all operations with 100% renewable energy by 2030, the Sustainability team has developed an on-site solar strategy for rapid deployment. Amazon leverages a financing mechanism called a Power Purchase Agreement (PPA) to execute deals and deploy solar. Sustainability has negotiated Master Service Agreements (MSAs) with industry leading solar developers to provide turn-key solar projects to Amazon. Amazon will evaluate solar panel deployment at this location in-line with any unique regulatory requirements.

Installation of rooftop solar on an Amazon building can drive financial savings, reduce grid energy consumption, and decrease carbon emissions. As of year-end 2019, Amazon has over 90 on-site solar projects installed and generating electricity in North America, The Middle East, and Africa, totaling more than 130 Megawatts. Amazon has over 40 operational on-site solar PV systems in the United States and with a focus on Fulfillment and Sort Centers.

Transportation Demand Management (TDM)

There are several standard TDM measures Amazon is taking for its delivery stations:

1. Pre-Tax Transit Pass Benefits – All AMZL Associates are able to use WageWorks to purchase transit passes or other approved commuter expenses pre-tax.
2. Carpool and Vanpool Ride-Matching Services – AMZL promotes both Waze Carpool and local carpool/vanpool ride-matching services wherever available.
3. Bicycle Parking – Each Delivery Station offers employee bike racks and day use lockers.
4. Guaranteed Ride Home (GRH) Program – AMZL offers a free GRH program for employees arriving to work by carpool, vanpool, or transit and need to leave work early or are unable to use their normal commute accommodations.
5. Designated Employee Transportation Coordinator (ETC) – Each Delivery Station has a designated ETC to answer employee commute related questions and advertise any available commuter benefits.

Community Engagement

Our associates in this region are also your residents and we want to ensure we are being good neighbors. To assist in this effort, Amazon has created a new community engagement team to work specifically with communities that have new delivery stations to forge new partnerships. Amazon offers several programs that will impact the Community in various ways. They include:

- Amazon Future Engineer – Amazon Future Engineer is a four-part, childhood-to-career program aimed at inspiring and educating hundreds of thousands of students from underrepresented and underserved communities each year to try computer science and coding. The program has an online application process that can be found at the following web address: <https://www.amazonfutureengineer.com/>
- School donations/giving – Amazon in the Community Programs makes community-based donations to a variety of programs and organizations throughout the United States.

It is our desire to have such a positive impact that you feel like this community has thrived as a result of our presence and associates in our delivery stations and drivers on the road are members of this community and share this same passion.

Workplace Conditions

Ensuring the safety of associates, drivers, and members of the communities in which we operate is our number one priority. Operational meetings, new hire orientation, process training and new process development begin with safety and have safety metrics and audits integrated within each program.

Training is constant, both in making sure employees know how best to work with the technology in the facility and also how to prevent injuries. While any serious incident is one too many, we learn and improve our programs working to prevent future incidents.

Our Delivery Stations are typical for the logistics industry where due to loading of vehicles, associates are often inside and outside as part of their shift. We closely monitor the temperatures in the stations to ensure a safe work environment and we also have industrial-grade fans, cooling mists, an abundance of water and other measures to ensure the safety of those at the site.

COVID-19

We are in unprecedented times and are taking exceptional measures to ensure the safety of our associates and communities during the COVID crisis, so that we can bring essential goods to those in need.

Amazon's facilities follow all health and safety protocols including social distancing guidelines. Across operations, Amazon has invested more than \$800 million dollars in the first half of this year on safety measures like temperature checks, masks, enhanced cleaning, gloves, sanitation stations, and on-site testing, to name a few.

We've made over 150 process updates to help protect employees, have implemented enhanced cleaning and social distancing measures, and we're piloting new efforts like using disinfectant fog in our New York fulfillment center. We've already had 175,000 new hires start new positions opening up to help meet customer demand and assist existing employees fulfilling orders for essential products.

Since the beginning of COVID-19, Amazon has made personal protective gear such as masks for our hourly employees and implemented temperature checks across our operations worldwide. Leadership at all levels in Amazon are working day and night every day continuously monitoring the effectiveness of the changes we made in the network and looking for opportunities for continuous improvement. *We are also looking for ways to leverage technology to help us audit social distance compliance.* We know this new normal is a big change for everyone and recognize the importance of inspecting our processes and looking for ways to make it easier to comply and ensure safety. Our facility's design, intended to make a more social distance-friendly environment, is an additional program element in alignment with occupancy requirements at this location. However, irrespective of this global health emergency, both at project launch and future peak operations, this facility will comply with all required building regulations.



**2677 E. ALESSANDRO BLVD
RIVERSIDE, CA 92553
DFX5 - DESIGN INTENT AND SCOPE**

Date: December 28, 2020

Project Data:

Developed Site: 39.23 AC

Building Size: 706,081 SF

Office TI: +/-18,715 SF (Proposed)

Existing TI: 1st Floor Office: 3,915 SF / 2nd Floor Mezzanine: 3,000 SF

Summary:

This site consists of recently completed construction including site improvements and adjacent public improvements previously approved by all AHJ per plot plan 14-02 including Resolution JPA# 18-03. The existing conditions include a new 706,081 sf concrete tilt-up building shell; a 3,915 sf speculative office build out, a 3,000 sf mezzanine, two single use unisex restrooms in warehouse and associated site improvements including but not limited to; site lighting, landscaping, parking striping, and ADA parking/accessible routes. Building is currently served with 4000A single service permanent power.

The proposed scope of work is a first-generation tenant improvement project which will occupy the entire building. Approximately 1/3 of the floor area will be dedicated to interior van parking and van loading area. Vans will park in this area overnight, but the parking area will be typically empty during the day when the vans are on their daily delivery routes.

215,000 sf of floor area is dedicated to package sortation and material handling operations. Packages arriving at this facility are prepackaged in cardboard boxes and labeled for shipping within local neighborhoods and areas surrounding the facility. The packages are brought into the facility via tractor trailer trucks which will utilize only the designated loading dock positions. Once unloaded into the facility associates sort the packages by routes, place the packages onto movable racks and load the packages into the delivery vans. Sorting primarily occurs between the hours of 10:00 PM to 8:00 AM. Once loaded, the vans will exit the building via an existing drive-out door and ramp along with two new prefabricated ramps (per plan) and exit the site and onto Old 215 Frontage Road. The vans will depart the facility in "waves" beginning at approximately 10:00am and continuing every 30 minutes until approximately 1:00pm. Van deployment is intentionally staggered and scheduled outside of morning peak hours in order to minimize impact to surrounding traffic. Vans will return to the facility following the completion of their daily deliveries. Van arrival is anticipated to occur intermittently between 7:00pm and 10:00pm each evening.

In addition to the interior parking and parcel delivery operations the proposed scope of work also includes 18,715 sf of accessory office space to support the facility. Please note that the overall square footage of the office area is significantly larger than would typically be implemented in a facility of this size due to the ongoing public health crisis caused by the COVID-19 pandemic. The public areas of the office including the training rooms, restrooms, and break area have been enlarged to accommodate proper social distancing and additional programmatic spaces such as the employee screening area have been added to ensure that all current health and safety protocols are implemented daily for as long as required.

A detailed list of the proposed improvements has also been included for reference;

Proposed Site Scope:

- New Parking Lot Striping to Accommodate Van Parking and Circulation
- New speed bumps/tables and directional striping per safety plan
- AMZL standard site directional and operational signage
- Surface mounted bollards in strategic locations based on van traffic flow
- Confirm ADA site compliance along accessible routes (no changes proposed)
- Ground mounted signage at entry (permit by signage vendor)
- Building mounted AMZL signage on façade (permit by signage vendor)
- Two (2) new prefabricated 12' wide "Dockzilla drive-out ramps per site plan
- New pedestrian sidewalk along West and North elevation, creating a pedestrian path of travel from the associate parking on the backside of the building to the main entrance.
- New striping required to accommodate accessible path of travel along the North elevation.
- Addition of accessory site structures (smoker's shelter & ride share shelter)

Proposed Building Shell Modifications:

- Two existing opening will be widened at the van exit locations to accommodate van traffic
- New 12'-0" wide overhead coiling doors installed at new opening locations
- Demolition of existing canopy and addition of larger entry canopy to existing façade
- Add Prime Blue accent paint to building entries per colored elevations provided
- Additional openings in roof, to accommodate new rooftop mechanical equipment per engineered Mechanical design.

Proposed TI Scope:

- Installation of new material handling conveyor system Per AMZL
- New remote restrooms, and scrubber dump
- Complete MEP design to accommodate delivery center operations. MEP scope includes but is not limited to;
 - New warehouse lighting
 - New rooftop mechanical units to accommodate warehouse heating and cooling
 - New exhaust fans and CO2 mitigation as required by local jurisdiction
 - New HVLS fans per AMZL standards
 - New plumbing for remote restrooms, handwashing sinks, and scrubber dump
 - Additional electrical distribution to support warehouse operations
- Install fire extinguishers throughout per local fire authority
- Interior parking striping
- Install new high-speed fabric roll-up doors on exterior of van entry/exit locations
- New dock package at line haul locations (dock seals, dock locks, lights, fans, etc.)
 - Dock package includes associated electrical work
- New dock levelers
- New full height partition, and associated doors separating interior parking from MHE operations
- Install interior chain link fence per AMZL - MHE plans
- Low Voltage/IT scope per IES Design
- Add AMZL standard restroom graphics to existing remote restrooms
- Fire sprinkler and Fire Alarm modifications as required to meet AMZL standards
- Demolition of existing speculative office area (including 3,000 sf office mezzanine) and construction of new +/- 18,715 sf office package per plan.

RECORDING REQUESTED BY
PROFICIENCY 215 LLC
AND WHEN RECORDED RETURN TO:

Proficiency 215 LLC
11777 San Vicente Boulevard, Suite 780
Los Angeles, CA 90049
Attention: Jeffrey Trenton

**This document was electronically submitted
to the County of Riverside for recording**
Received by: ALYCIA #778

(Space Above For Recorder's Use)

COVENANT AFFECTING REAL PROPERTY

THIS COVENANT AFFECTING REAL PROPERTY ("Covenant") is made as of the 22nd day of August 2018, by Proficiency 215 LLC, a Delaware limited liability company ("Covenanter"), with reference to the following facts set forth in the recital paragraphs below:

-- RECITALS --

A. Covenanter is the owner of the real property described more particularly in Exhibit A hereto ("Property"), subject to the jurisdiction of the March Joint Powers Authority ("JPA").

B. The Property is situated approximately 5,440 feet northwesterly of the northwesterly terminus of runway 14-32 located at March Air Reserve Base ("MARB") and is subject to the Air Installation Compatible Use Zone Study ("AICUZ") and the March ARB/Inland Port Airport Land Use Compatibility Plan ("ALUCP"). The AICUZ and ALUCP have designated one portion of the Property as being located in Accident Potential Zone ("APZ") I and the remainder of the Property as being located in APZ II.

C. Portions of the Property are designated in APZ I and have an occupancy limit of 25 persons per acre. Other portions of the Property are designated in APZ II and have an occupancy limit of 50 persons per acre.

D. The Covenanter is developing the Property for an industrial warehouse building. The Covenanter has submitted to the JPA three applications for discretionary land use approvals for the development of the Property including, 1) a General Plan Amendment, assigning the JPA's General Plan designation of Industrial (I) to the Property; 2) a Change of Zone, assigning the zoning designation of Industrial (I) to the Property, consistent with the General Plan designation of Industrial (I); and 3) a Plot Plan for the proposed development of an approximately 709,083-square-foot industrial warehouse building; and environmental documentation pursuant to the California Environmental Quality Act ("CEQA"). All of the above shall be referred to collectively as the "Project."

87267-49

E. Covenanter has agreed to comply with the Density Restrictions and a Density Cap (both terms are defined below), by limiting occupancy of the Project to (i) five hundred and eighteen (518) occupants ("Density Cap") [THE DENSITY CAP WILL DECREASE IF THE SQUARE FOOTAGE OF THE BUILDING DECREASES.]; (ii) twenty-five (25) occupants in any square area measuring 208 feet by 208 feet ("Square Area") for all Square Areas within portions of the building of the Project within APZ I; and (iii) fifty (50) occupants in any Square Area within portions of the building of the Project within APZ II. Requirements (ii) and (iii) are collectively the "Density Restrictions," and are depicted in Exhibit B, attached hereto and incorporated herein by reference. Accordingly, any building expansion is prohibited, including an increase in the building mezzanine area, without further review by JPA and MARB representatives, and consent and approval provided through an amendment to this Covenant.

F. JPA requires that the Covenanter, and each of its successors in interest in the Property, comply with the Density Cap and Density Restrictions.

G. JPA has indicated that it does not have the resources to undertake regular inspections of the Project to determine compliance with the Density Cap and Density Restrictions.

H. In order to ensure that Covenanter complies with the Density Cap and Density Restrictions, upon occupancy of the Project, Covenanter agrees to fund at its sole expense quarterly inspections. These inspections shall be undertaken by a neutral independent third party to be selected by JPA ("Independent Monitor"). The JPA shall provide invoices to Covenanter concerning the Independent Monitor's costs in undertaking the activities specified in the Covenant and such costs shall be reasonable and reflect the Independent Monitor's actual costs of inspection and reporting.

I. In order to ensure that there is compliance with the Density Cap and Density Restrictions, the Covenanter agrees to the provisions as set forth below during the term of the Covenant ("Covenant Lifespan").

-- AGREEMENT --

ARTICLE 1
GENERAL PROVISIONS

1.1 **Provisions to Run with the Land.** Subject to the conditions of this Covenant, during the Project Lifespan, the Density Cap and Density Restrictions shall be binding upon the Covenanter and its successors and assigns in interest in the Property (collectively "Covenanter"). The Covenant, the Density Cap, and Density Restrictions shall run with the Property and each portion thereof, and shall apply to and bind the Covenanter during the Project Lifespan.

1.2 **Deemed Concurrence.** The Covenanter and all other persons or entities acquiring any fee interest in the Property, shall be conclusively deemed by such acquisition to have irrevocably agreed to the Density Cap and Density Restrictions for and among themselves and their heirs, successors, and assigns. Upon any such person's or entity's sale or transfer of its interest in the Property, such conveying person or entity shall forever be released and relieved of any further obligation or liability arising under this Covenant, the Density Cap, and the Density Restrictions for events arising from and after the date of such transfer.

1.3 Incorporation into Deeds and Leases. The Density Cap and Density Restrictions are hereby deemed to be incorporated by reference into each conveyance of any fee or leasehold interest in the Property or portion thereof occurring after the date the Covenant is recorded in the Official Records of Riverside County, California, whether or not referred to in the instrument affecting such conveyance of the Property, or a portion thereof.

1.4 Parties Benefitting. This Covenant inures to the benefit of MARB and JPA and their successors in interest or assigns.

ARTICLE 2
ENFORCEMENT OF COVENANT

2.1 Right of Entry. Upon completion and occupancy of the Project, Covenanter shall allow the Independent Monitor to enter the Project, for the sole purpose of calculating the Project occupancy and confirming that the Covenanter is complying with the Density Cap and Density Restrictions. The Independent Monitor's method for calculating the Project occupancy and confirming that the Covenanter is complying with the Density Cap and Density Restrictions shall include separate calculations of employment occupancy within APZ I and APZ II and shall be approved by JPA, at its sole discretion, prior to the commencement of inspections. Upon JPA approval of the Independent Monitor's method of calculation, JPA or the Independent Monitor shall notify the Covenanter of the Independent Monitor's methods of calculation in writing ("Methodology"), before the Independent Monitor inspects the Project, in order for Covenanter to understand the Methodology to be used by the Independent Monitor. The Methodology shall be consistent with and be based upon the calculations of the Density Restrictions. The Independent Monitor shall inspect the Project quarterly at times solely determined by the Independent Monitor within each calendar quarter. "Quarterly" shall mean each calendar quarter beginning on January 1, April 1, July 1, or October 1, as applicable, and ending on the succeeding March 31, June 30, September 30, or December 31, as applicable. Notwithstanding the foregoing, the Independent Monitor's inspections shall be done in a safe and unobtrusive manner. Within thirty (30) days of completing its quarterly inspection, the Independent Monitor shall submit written reports to MARB, JPA, and Covenanter documenting the Project occupancy. If the Independent Monitor determines that the Density Cap or Density Restrictions have been exceeded, within three (3) days of this determination, the Independent Monitor shall provide written notification to Covenanter, MARB, and JPA of this exceedance. Upon receipt of such notice, Covenanter shall eliminate the exceedance within fifteen (15) days. At the conclusion of this 15-day period, the Independent Monitor shall re-inspect the Project to determine whether the exceedance has been eliminated. If the exceedance has not been eliminated, the Independent Monitor shall provide written notification to Covenanter, MARB and JPA of this uncured exceedance no later than five (5) days thereafter and the Independent Monitor shall pursue the Administrative Remedies specified in Section 2.3 of this Covenant. If the exceedance has been eliminated, the Independent Monitor shall provide written notification no later than five (5) days thereafter to Covenanter, MARB and JPA of this elimination.

2.2 Remedies. The remedies specified in Section 2.3 of this Covenant shall be triggered upon either (i) one (1) exceedance of the Density Cap or Density Restrictions by Covenanter which has not been corrected within the cure period set forth above in Section 2.1 in any one (1) year period of Project occupancy; or (ii) two (2) exceedances of the Density Cap or Density Restrictions by Covenanter which have been eliminated within the cure period set forth above in Section 2.1 in any one (1) year period of Project occupancy ("Actionable Default").

2.3 Administrative Remedies. Upon any Actionable Default, the Independent Monitor shall notify Covenanter, JPA, and MARB in writing of the Actionable Default and JPA shall commence the revocation process of the Project's certificate of occupancy pursuant to JPA Development Code § 9.02.260. If the Project's certificate of occupancy is revoked pursuant to this process, all operations and/or activities at the Project site shall immediately terminate. If the certificate of occupancy is revoked, Covenanter may reapply to JPA to re-establish the use and occupancy of the Project, subject to compliance with the Density Cap and Density Restrictions. JPA may authorize a re-establishment of the use and occupancy of the Project if Covenanter provides reasonable assurances to JPA that the Density Cap and Density Restrictions will not be exceeded in the future.

2.4 Mortgagee Protection. Notwithstanding any other provision of this Covenant, no breach of the Density Cap or Density Restrictions, nor the enforcement of any provisions contained in this Covenant shall affect, impair, or defeat the lien or charge of any duly recorded mortgage or deed of trust encumbering any portion of the Property, or affect, impair, or defeat the interest of the mortgagee, or its successor or assigns pursuant to such a mortgage, provided that such mortgage is made in good faith and for value. The Density Cap and Density Restrictions shall be binding upon and effective against any person whose title in the Property or any portion thereof, is derived through foreclosure, deed in lieu of foreclosure, or trustee's sale during the period of their ownership.

ARTICLE 3
MISCELLANEOUS

3.1 No Dedication Intended. Nothing herein shall be construed to be a grant or dedication, or offer to grant or dedicate, the Property or any portion thereof to MARB or JPA for any purposes whatsoever.

3.2 Notices.

(a) All notices and communications relating to this Covenant shall be in writing and shall be deemed effective when such notice or communication is personally delivered to the person:

To Covenanter: Proficiency 215 LLC
11777 San Vicente Blvd., Suite 780
Los Angeles, CA 90049
Attention: Jeffrey Trenton

and to:

Allen Matkins Leck Gamble Mallory & Natsis LLP
1900 Main Street, 5th Floor
Irvine, CA 92614
Attention: John Condas, Esq.

To JPA: March Joint Powers Authority
 14205 Meridian Parkway, Suite 140
 Riverside, CA 92518
 Attention: Danielle Wheeler

and to:

Best Best & Krieger, LLP
 3390 University Avenue, 5th Floor
 Riverside, CA 92501
 Attention: Charity B. Schiller

(b) Concurrently with the consummation of the conveyance of an interest in the Property, or portion thereof, by the Covenanter to a successor or assign of such Covenanter, such transferee shall notify the Independent Monitor and JPA in writing of such conveyance. Such notification shall set forth the name of the transferee and its contact information for the purposes of the giving notice to such transferee under Section 3.2(a). Prior to the receipt by the Independent Monitor of any such notification, any and all written communication by the Independent Monitor under this Covenant shall be sufficient if given to the address for the Covenanter as provided in Section 3.2(a).

3.3 Breach. For all breaches of this Covenant which are not an Actionable Default, failure by Covenanter to perform an obligation which arises hereunder shall constitute a breach of this Covenant. Upon a breach of this Covenant, JPA shall commence the revocation process of the Project certificate of compliance as set forth above in Section 2.3.

3.4 Partial Invalidity. If any portion of the Covenant is determined by a judgment of a court of competent jurisdiction to be invalid for any reason, the remaining portions shall remain in full force and effect as if such portions had not been included in such a judgment.

3.5 Successors and Assigns of Covenanter. This Agreement shall be binding upon the successors in interest and assigns of Covenanter.

3.6 Covenant Lifespan. As used herein the words "Covenant Lifespan" refer to the period of time following the date of recordation of this Covenant when the Covenant is in effect. The Covenant Lifespan shall have a term of the earliest of (i) demolition of the Project; (ii) the MARB and March Inland Port Airport Authority, or subsequent civilian airport, ceasing operations as an airport; (iii) written request by MARB and March Inland Port Airport Authority to terminate the Covenant; or (iv) removal of the Project from APZ I and APZ II.

3.7 Initiation of Enforcement Proceedings. Notwithstanding any other provision of this Covenant to the contrary, no third party other than JPA may compel enforcement of any provision of this Covenant.

3.8 Jurisdiction and Venue. All legal actions arising from this Covenant shall be filed in the Superior Court of the State of California in and for the County of Riverside, California, or the United States District Court with jurisdiction in the County of Riverside, California.

3.9 Time is of the Essence. Time is of the essence in performance of the obligations set forth in this Covenant.

IN WITNESS WHEREOF, Covenanter has executed this Covenant as of the date set forth above as evidenced by the authorized officer of Covenanter whose signature appears below.

COVENANTER
Proficiency 215 LLC,
a Delaware limited liability company

By: 
Jeffrey N. Trenton
Its: President

ACKNOWLEDGMENT

A notary public or other officer completing this certificate verifies only the identity of the individual who signed the document to which this certificate is attached, and not the truthfulness, accuracy, or validity of that document.

State of California)
County of Los Angeles)

On Aug. 22, 2018, before me, Mark C Glode,

Notary Public, personally appeared JEFFREY N. THENTON (insert name of notary), who proved to me on the basis of satisfactory evidence to be the person(s) whose name(s) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

I certify under PENALTY OF PERJURY under the laws of the State of California that the foregoing paragraph is true and correct.

WITNESS my hand and official seal.

Signature _____

(Seal)

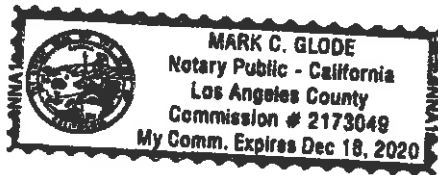


EXHIBIT A
LEGAL DESCRIPTION OF THE PROPERTY

LEGAL DESCRIPTION

THE LAND REFERRED TO HEREIN BELOW IS SITUATED IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS:

PARCEL A: (APN: 297-100-045-8)

THAT PORTION OF SECTION 15, TOWNSHIP 3 SOUTH, RANGE 4 WEST, SAN BERNARDINO MERIDIAN, IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, ACCORDING TO THE OFFICIAL PLAT THEREOF; CONVEYED TO THE CALIFORNIA SOUTHERN RAILROAD COMPANY BY DEED RECORDED JANUARY 23, 1888, IN BOOK 69, PAGE 91 OF DEEDS, IN THE OFFICE OF THE COUNTY RECORDER OF SAN BERNARDINO COUNTY, CALIFORNIA, DESCRIBED AS FOLLOWS:

COMMENCING AT THE NORTHEAST CORNER OF SAID SECTION 15, MARKED BY A 1-1/2 INCH BRASS DISC, STAMPED "RIV CO SUR-1985-SEC COR"; THENCE ALONG THE NORTH LINE OF SAID SECTION 15, NORTH 89° 52' 19" WEST, 1,461.68 FEET TO THE EASTERLY RIGHT OF WAY LINE OF THE ATCHISON, TOPEKA AND SANTA FE RAILWAY (FORMERLY CALIFORNIA SOUTHERN RAILROAD COMPANY) PER ABOVE SAID DEED AND TO THE POINT OF BEGINNING; THENCE COURSE "A", ALONG SAID EASTERLY RIGHT OF WAY LINE, SOUTH 19° 23' 12" EAST, 2678.25 FEET TO THE INTERSECTION OF THE WESTERLY PROLONGATION OF THE NORTH LINE OF THAT CERTAIN PARCEL OF LAND ACQUIRED BY THE UNITED STATES OF AMERICA BY DECREE OF TAKING, A CERTIFIED COPY OF WHICH WAS RECORDED MARCH 2, 1942, IN BOOK 532, PAGE 311, OFFICIAL RECORDS OF RIVERSIDE COUNTY, AND BY DECREE ON AMENDED DECLARATION OF TAKING, A CERTIFIED COPY OF WHICH WAS RECORDED FEBRUARY 24, 1943, IN BOOK 571, PAGE 237, OFFICIAL RECORDS OF RIVERSIDE COUNTY, WITH THE WESTERLY RIGHT OF WAY LINE OF THAT CERTAIN PARCEL OF LAND ACQUIRED BY THE STATE OF CALIFORNIA (STATE ROUTE 215), AS PARCEL NO. 35, IN DECREE OF CONDEMNATION, A CERTIFIED COPY OF WHICH WAS RECORDED MAY 18, 1943, IN BOOK 580, PAGE 327, OFFICIAL RECORDS OF RIVERSIDE COUNTY; THENCE CONTINUING ALONG SAID EASTERLY RIGHT OF WAY LINE AND SAID WESTERLY RIGHT OF WAY LINE (STATE ROUTE 215), SOUTH 19° 23' 12" EAST, 82.79 FEET TO THE BEGINNING OF A NON-TANGENT CURVE, CONCAVE NORTHEASTERLY, HAVING A RADIUS OF 875.00 FEET; THENCE NORTHWESTERLY ALONG SAID CURVE, FROM A TANGENT BEARING OF NORTH 66° 20' 39" WEST, THROUGH A CENTRAL ANGLE OF 9° 47' 09", AN ARC LENGTH OF 149.44 FEET TO THE WESTERLY RIGHT OF WAY LINE OF SAID ATCHISON, TOPEKA AND SANTA FE RAILWAY, SAID LINE BEING PARALLEL WITH AND 100.00 FEET SOUTHWESTERLY MEASURED AT RIGHT ANGLES TO THE ABOVE MENTIONED COURSE "A"; THENCE ALONG SAID WESTERLY RAILWAY RIGHT OF WAY LINE, NORTH 19° 23' 12" WEST, 2685.67 FEET TO SAID NORTH LINE OF SECTION 15; THENCE ALONG SAID NORTH LINE, SOUTH 89° 52' 19" EAST, 106.10 FEET TO THE POINT OF BEGINNING.

PARCEL B: (APN: 297-100-013-9)

PARCEL 3 (EAST): BEING THAT PORTION OF SECTION 15, TOWNSHIP 3 SOUTH, RANGE 4 WEST, SAN BERNARDINO BASE AND MERIDIAN, ALSO SHOWN AS PARCEL 3 OF RECORD OF SURVEY 000-135, ON FILE IN BOOK 110, PAGES 30 THROUGH 40, INCLUSIVE, OF RECORDS OF SURVEY, RECORDS OF RIVERSIDE COUNTY, CALIFORNIA, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHERLY TERMINUS OF THAT CERTAIN COURSE IN THE WESTERLY LINE OF THE ATCHISON, TOPEKA AND SANTA FE RAILROAD RIGHT-OF-WAY AS SHOWN ON SHEET 7 OF 11, SHEETS OF SAID RECORDS OF SURVEY, RECORDS OF RIVERSIDE COUNTY, CALIFORNIA, SAID COURSE BEARS SOUTH 19° 23' 04" EAST, 2,577.64 FEET, ALSO BEING SHOWN ON CALIFORNIA DEPARTMENT OF TRANSPORTATION MAP NO. 435571-8, ON FILE WITH THE COUNTY OF RIVERSIDE MAP NO. 205-254;

THENCE SOUTH 19° 23' 04" EAST, 2,577.64 FEET TO THE BEGINNING OF A NON-TANGENT CURVE CONCAVE NORTHEASTERLY HAVING A RADIUS OF 875.00 FEET, A RADIAL LINE TO SAID

BEGINNING BEARS SOUTH 33° 26' 41" WEST, SAID BEGINNING ALSO BEING A POINT ON THE EASTERLY RIGHT-OF-WAY LINE OF STATE ROUTE 215, AS SHOWN ON CALIFORNIA DEPARTMENT OF TRANSPORTATION MAP NO. 435571-6, ON FILE WITH THE COUNTY OF RIVERSIDE MAP NO. 205-251;

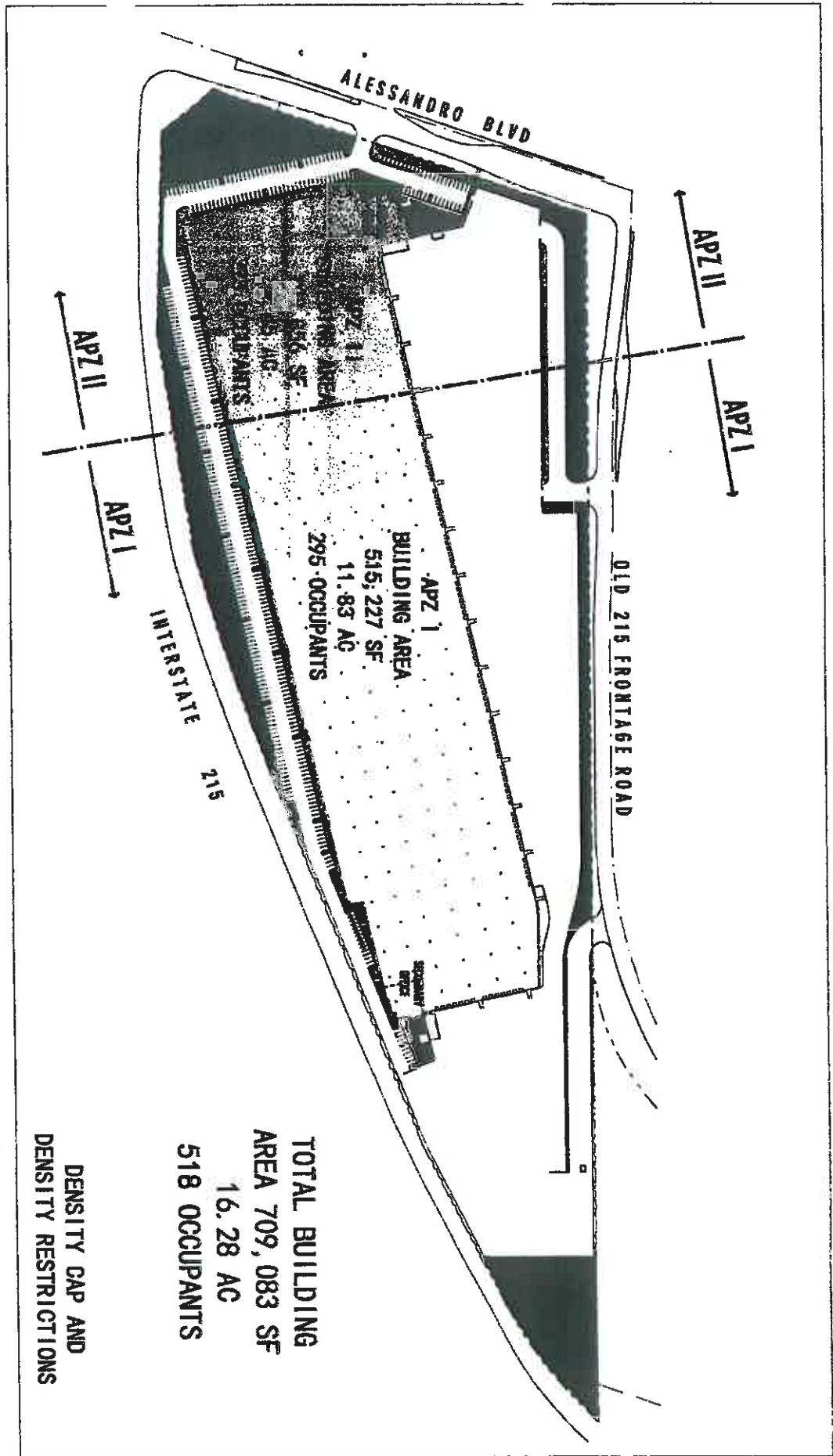
THENCE ALONG SAID EASTERLY LINE THE FOLLOWING 9 COURSES:

- 1) NORTHWESTERLY 171.38 FEET ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 11° 13' 20";
- 2) NORTH 45° 20' 00" WEST, 391.10 FEET;
- 3) NORTH 40° 30' 27" WEST, 878.53 FEET TO THE BEGINNING OF A CURVE CONCAVE NORTHEASTERLY HAVING A RADIUS OF 4,875.00 FEET;
- 4) NORTHWESTERLY ALONG SAID CURVE 508.50 FEET THROUGH A CENTRAL ANGLE OF 05° 58' 35";
- 5) NORTH 28° 08' 58" WEST, 486.75 FEET;
- 6) NORTH 17° 49' 57" WEST, 447.33 FEET;
- 7) NORTH 30° 29' 16" EAST, 142.45 FEET;
- 8) NORTH 89° 54' 38" EAST, 415.29 FEET TO THE SOUTHERLY LINE OF ALESSANDRO BOULEVARD AS SHOWN ON CALIFORNIA DEPARTMENT OF TRANSPORTATION MAP NO. 435571-8, ON FILE WITH THE COUNTY OF RIVERSIDE MAP NO. 205-254;
- 9) THENCE ALONG SAID SOUTHERLY LINE SOUTH 89° 51' 58" EAST, 314.75 FEET TO THE POINT OF BEGINNING.

RESERVING THEREFROM ALL OIL, GAS AND OTHER MINERAL RESOURCES OF ANY KIND OR NATURE IN THE MINERAL ESTATE OF THE PROPERTY, PROVIDED, HOWEVER, THAT SUCH RESERVATION SHALL NOT INCLUDE THE RIGHT OF ACCESS TO OR ANY RIGHT TO USE ANY PORTION OF THE SURFACE OF THE PROPERTY, AS RESERVED IN DEED RECORDED DECEMBER 14, 2001, AS INSTRUMENT NO. 2001-622399, AND JUNE 25, 2002, AS INSTRUMENT NO. 2002-347891, BOTH OF OFFICIAL RECORDS.

APNs: 297-100-045-8 and 297-100-013-9

EXHIBIT B
DENSITY CAP AND DENSITY RESTRICTIONS



NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893**. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The March Joint Powers Authority should be contacted on non-ALUC issues. For more information please contact March Joint Powers Authority Planner Mr. Jeffrey Smith at (951) 656-7000.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to prull@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please telephone Barbara Santos at (951) 955-5132.

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside California

DATE OF HEARING: March 11, 2021

TIME OF HEARING: 9:30 A.M.

Pursuant to Executive Order N-25-20, this meeting will be conducted by teleconference and at the Place of Hearing, as listed above. Public access to the meeting location will be allowed, but limited to comply with the Executive Order. Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org

CASE DESCRIPTION:

ZAP1444MA20 – Jared Riemer/PR III/CHI Freeway BC, LLC (Representative: Glassman Planning Associates, MG2, and MIG, Inc.) – March Joint Powers Authority Case No. PP14-02 (Plot Plan/Determination of Substantial Conformance No. 2). The applicant proposes to revise the occupancy use and floor plan of an existing (but vacant) 709,083 square foot high-cube industrial warehouse building to allow for a Delivery Parcel Hub facility on 39.42 acres, located southerly of Alessandro Boulevard, easterly of Interstate 215, westerly of Old 215 Frontage Road, and northerly of Cactus Avenue. There is no increase to the building's footprint. The building, as amended, would provide for 258,000 square feet of high-cube warehouse area, 8,000 square feet of office area, a 1,500 square foot break room, 126 loading/queuing van spaces, and 70 indoor van parking spaces. The original shell building project (ZAP1107MA14), which proposed 694,083 square feet of high-cube logistics warehouse, 12,000 square feet of first floor office area, and 3,000 square feet of second floor office mezzanine, was found consistent by ALUC in 2015. A subsequent revision to the high-cube warehouse building (ZAP1394MA19) was proposed to provide an additional 10,000 square feet of office area (by reducing warehouse area by the same square footage) was also found consistent by ALUC in 2020 (Airport Compatibility Zones B1-APZ-I and B1-APZ-II of the March Air Reserve Base/Inland Port Airport Influence Area).



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

Need new case #
ALUC CASE NUMBER: ~~PP 14-02~~ ZAP1444MAZD DATE SUBMITTED: 12/30/2020

APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

Applicant	Glassman Planning Associates / Contact: David Glassman	Phone Number	310-781-8250
Mailing Address	1111 Sartori Ave. Torrance, CA 90501	Email	david@gpan.com
Representative	MG2 / Contact: Mike Miranda	Phone Number	206-962-6589
Mailing Address	1101 2nd Ave #100 Seattle, WA 98101	Email	mike.miranda@mg2.com
Property Owner	PR III - CHI Freeway BC / Contact: Jared Riemer	Phone Number	213-631-2336
Mailing Address	527 W 7th St, Ste #308 Los Angeles, CA 90014	Email	jriemer@chindustrial.com

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LOCAL JURISDICTION AGENCY

Local Agency Name	March Joint Powers Authority	Phone Number	951-807-7283
Staff Contact	Jeff Smith	Email	
Mailing Address	14205 Meridian Parkway, Suite 140	Case Type	Condition Use Permit
Local Agency Project No	PLOT PLAN 14-02 - SUBSTANTIAL CHANGE 2	<input type="checkbox"/>	General Plan / Specific Plan Amendment
		<input type="checkbox"/>	Zoning Ordinance Amendment
		<input type="checkbox"/>	Subdivision Parcel Map / Tentative Tract
		<input checked="" type="checkbox"/>	Use Permit
		<input type="checkbox"/>	Site Plan Review/Plot Plan
		<input type="checkbox"/>	Other

PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address	2677 E Alessandro Blvd, Riverside, CA 92553	Gross Parcel Size	39.23 AC
Assessor's Parcel No.	297-100-045, -087	Nearest Airport and distance from Airport	Riverside Municipal Airport (10.6 MBoe)
Subdivision Name	N 1/2 OF SEC 15 T.3S., R.4W		
Lot Number	87 / 45		

PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe)	The site currently consists of a recently completed speculative industrial building, and associated site and public improvements as previously approved per Resolution #JPA 18-03 and Plot Plan 14-02. The building includes a speculative office area, including a 3,000 sf mezzanine, and two remote warehouse restrooms, but the building is currently vacant.
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Proposed Land Use (describe)	The proposed scope of work is a first generation tenant improvement. The proposed use is a Parcel Delivery Station which is an allowed use per Chapter 9.02 / Table 9.02.020-1 of the March JPA Development Guidelines. The tenant space will occupy the entire 706,081 sf of building area including 252 interior parking stalls, 215,000 sf of parcel delivery operations, & 18,715 sf of office which will replace the existing speculative office area.		
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)	N/A	
For Other Land Uses (See Appendix C)	Hours of Operation	24 Hours	
	Number of People on Site	172	Maximum Number 298
	Method of Calculation	172 Max Employees Per Tenant Operations + 126 Interior Van Loading Spaces = 298 Total Max Occupants.	
Height Data	Site Elevation (above mean sea level)	FFE = -0.67'	ft.
	Height of buildings or structures (from the ground)	44'-6" (Existing)	ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No
	If yes, describe	_____	

- A. **NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. **REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. **SUBMISSION PACKAGE:**
1. Completed ALUC Application Form
 1. ALUC fee payment Total Fee = \$4,021 (See Fee Schedule for Additional Information)
 1. Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
 1. Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
 1. CD with digital files of the plans (pdf)
 1. Vicinity Map (8.5x11)
 1. Detailed project description
 1. Local jurisdiction project transmittal Previously Provided By Jeff Smith - March JPA
 3. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
 3. Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. (Only required if the project is scheduled for a public hearing Commission meeting)

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3-5 2.2

HEARING DATE: May 13, 2021 (Continued from April 8, 2021)

CASE NUMBER: ZAP1456MA21 – Penske Truck Leasing Co. L.P/CD Moreno Holding LLC)

APPROVING JURISDICTION: City of Moreno Valley

JURISDICTION CASE NO: PEN21-0025 (Plot Plan)

LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

Airport Influence Area: March Air Reserve Base

Land Use Policy: Zones B1-APZ-I and B1-APZ-II

Noise Levels: 65-70 CNEL

MAJOR ISSUES: *The project was continued from the April 8, 2021, meeting, pending a new analysis of the revised site design introduced by the applicant on March 25, 2021 (from one building to two buildings). This new site layout has been reviewed and analyzed in this staff report and was re-advertised to the public.*

Also, at the April 8, 2021, meeting, the Air Force provided their comment letter dated April 6, 2021. The letter identified that the use of the Covenant was encouraged by the Air Force, with the ultimate solution being the completion of the March Air Reserve Base Compatible Use Study and the subsequent update to the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, which addresses the intensity inconsistency between the 2014 March ALUCP and the Department of Defense Instruction No. 4165.57.

The applicant is proposing a revised project and site design layout from what was previously reviewed and found consistent by the Commission in ALUC project's ZAP1038MA07 and ZAP1333MA18. A breakdown of the proposed Penske Truck rental and repair facility indicates that the proposed project's average and single acre intensities are consistent with the Compatibility Zone B1-APZ-I criteria.

However, the resulting project intensity exceeds the Air Force's interpretation of Air Force Instruction 32-7063 dated December 18, 2015, which addresses Air Force policies on Land Use Compatibility in accordance with Department of Defense Instruction (DoDI) No. 4165.57, for

APZ-I and APZ-II. The Air Force understands the DoDI as limiting intensity to a maximum of 25 people in any given acre in APZ-I, and a maximum of 50 people in any given acre in APZ-II.

In order to address this issue, the applicant has agreed to a condition requiring the recording of a Covenant on the title of the property, restricting actual occupancy of the building to a maximum of 25 people in any given acre in APZ-I. Operation in compliance with this covenant will be necessary to satisfy Air Force and City of Moreno Valley concerns regarding project intensity.

RECOMMENDATION: Staff recommends that the Plot Plan be found *CONSISTENT*, subject to the conditions included herein. ~~CONTINUE the matter to the May 13 June 10, 2021, meeting, pending completion of the Air Force review of the project.~~

PROJECT DESCRIPTION: The applicant proposes to construct a ~~21,155 square foot~~ Penske Truck facility including a **19,200 square foot building, a 1,792 square foot building and a 1,936 square foot truck fueling island on 9.10 acres.**

On October 11, 2007, the Commission found the original 16-building (totaling 253,740 square feet) business park on 19.56 acres (ZAP1038MA07), consistent with the 1984 Riverside County Airport Land Use Plan, as applied to March Air Reserve Base/Inland Port Airport.

On December 18, 2018, the Commission found a proposal to construct 10 industrial warehouse buildings (totaling 122,979 square feet) on 9.37 acres, and a proposal to develop a 15,280 square foot Penske Truck building on 9.86 acres, consistent with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan.

PROJECT LOCATION: The site is located southerly of Alessandro Boulevard, westerly of Day Street, and easterly of Old Highway 215 Frontage Road, within the City of Moreno Valley, approximately 6,350 feet northwesterly of the northerly end of Runway 14-32 at March Air Reserve Base.

BACKGROUND:

Non-Residential Average Land Use Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zones B1-APZ-I (8.56 acres) and B1-APZ-II (0.36 acres). Zone B1-APZ-I limits average intensity to 25 people per acre, and APZ-II limits average intensity to 50 people per acre. The majority of the site's development footprint (**two buildings, truck fuel island**) is located within Zone B1-APZ-I, whereas truck parking and landscaping are located within Zone B1-APZ-II.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan and the Additional Compatibility Policies included in the March ALUCP, the following rates were used to calculate the occupancy for the proposed project:

- Assembly (conference room, breakroom, lobby) – 1 person per 15 square feet,
- Office – 1 person per 200 square feet,
- Storage – 1 person per 300 square feet,
- Warehouse – 1 person per 500 square feet, and
- Service Bay/Wash Bay/Truck Fueling spaces – 1.5 persons per bay door/fueling spaces.

The proposed project **two buildings** includes a **total of 1,048 square feet of office area, 1,797 square feet of warehouse area, 2,830 square feet of storage area, 326 square feet of break room area, 420 square feet of lobby area, 192 square feet of conference room area**, 5 service repair bay doors, 1 wash bay door, and two truck fueling spaces, ~~433 square feet of lobby area, 1,995 square feet of office area, 1,644 square feet of warehouse area, 1,208 square feet of storage area, 348 square feet of break room area, 180 square feet of conference room area~~ accommodating an occupancy of ~~93~~ **94** people, resulting in an average intensity of ~~10~~ **11** people per acre, which is consistent with the Zone B1-APZ-I average criterion of 25 people per acre.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per standard vehicle and 1.0 persons per trailer truck in the absence of more precise data). Based on the number of parking spaces provided (~~46~~ **44** standard vehicles, ~~141~~ **148** truck spaces), accommodating a total occupancy of ~~210~~ **214** people, resulting in an average intensity of ~~23~~ **25** people per acre, which is consistent with the Zone B1-APZ-I average criterion of 25 people per acre.

Non-Residential Single-Acre Land Use Intensity: Compatibility Zones B1-APZ-I and APZ-II limit maximum single-acre intensity to 100 people. There are no risk-reduction design bonuses available, as March Air Reserve Base/Inland Port Airport is primarily utilized by large aircraft weighing more than 12,500 pounds.

Based on the **revised** site plan provided and the occupancies as previously noted, the maximum single-acre intensity in B1-APZ-I is **located around building 1 and includes 639 square feet of office area, 1,797 square feet of warehouse area, 2,564 square feet of storage area, 326 square feet of break room area, 1 wash bay**, 5 service repair bay doors, and two truck fueling spaces, ~~433 square feet of lobby area, 1,995 square feet of office area, 1,644 square feet of warehouse area, 1,208 square feet of storage area, 348 square feet of break room area, 180 square feet of conference room area~~ accommodating a total occupancy of ~~92~~ **50** people, which is consistent with the Compatibility Zone B1-APZ-I single acre intensity criterion of 100 .

Although the abovementioned single acre intensity in B1-APZ-I is consistent with the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, it is inconsistent with the Air Force Department of Defense Instruction No. 4165.57 with regards to intensity, which is limited to a maximum of 25 people in any given acre in APZ-I, and 50 people in APZ-II. A more detailed analysis is provided below in the March Air Reserve Base section of the staff report.

March Air Reserve Base/United States Air Force Input: Given that the project site is located in Zones B1-APZ-I and B1-APZ-II of the primary runway at March Air Reserve Base, the March Air

Reserve Base staff was notified of the project and sent a package of plans for their review. ~~As of the time this staff report was prepared, we were still awaiting comments from the Air Force regarding this project.~~

The 2018 Airport Installation Compatible Use Zones (AICUZ) study identifies the project site as located within Accident Potential Zone II (APZ-II). Appendix A of the AICUZ provides Land Use Compatibility Tables for the APZs, which cite “warehousing” and “repair services” as permitted uses in APZ-I and APZ-II (and prohibited use in the Clear Zone [CZ]).

However, March Air Reserve Base officials maintain that the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan is not consistent with current Air Force guidance found in Air Force Instruction 32-7063 dated December 18, 2015, which addresses Air Force policies on Land Use Compatibility in accordance with Department of Defense Instruction (DoDI) No. 4165.57. These inconsistencies include conflicts with regard to lot coverage, intensity, and permitted use definitions.

The proposed project complies with the restrictions on permitted uses and lot coverage, but not with the intensity limits. The Air Force understands the DoDI criteria as limiting intensity to a maximum of 25 people in any given acre in APZ-I and to a maximum of 50 people in any given acre in APZ-II. As noted above, the project would be expected to result in a single acre occupancy of ~~100~~ 50 people in ~~APZ-I APZ-II~~.

The projected occupancy intensities would be inconsistent with the Air Force intensity understanding.

One method of bringing the project into consistency with both the Air Force Instruction (AFI) is for the applicant to agree to a condition including a Covenant, recorded on the title of the property, restricting the actual occupancy of the building to the limits of the AFI.

The applicant has agreed to this condition, which limits actual occupancy of the building to 25 persons in any given acre within APZ-I and to 50 people persons in any given acre within APZ-II. Specifically, the Covenant states:

E. Covenanter has agreed to comply with the Density Restrictions and a Density Cap (both terms are defined below), by limiting occupancy of the Project to (i) twelve (12) occupants (“Density Cap”) [**THE DENSITY CAP WILL DECREASE IF THE SQUARE FOOTAGE OF THE BUILDING DECREASES.**]; (ii) twenty-five (25) occupants in any square area measuring 208 feet by 208 feet (“Square Area”) for all Square Areas within portions of the building of the Project within APZ I, and (iii) fifty (50) occupants in any Square Area within portions of the building of the Project within APZ II. Requirement (ii) and (iii) are collectively the “Density Restrictions”, and are depicted in Exhibit B, attached hereto and incorporated herein by reference. Accordingly, any building expansion is prohibited, including an increase in building area, without further review by the City and MARB representatives, and consent and approval provided through an amendment to this covenant.

The Air Force provided a comment letter dated April 6, 2021, which was submitted at the April 6, 2021, ALUC meeting. The letter identified that the use of the Covenant was encouraged by the Air Force, with the ultimate solution being the completion of the March Air Reserve Compatible Use Study and the subsequent update to the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, which addresses the intensity inconsistency between the 2014 March ALUCP and the Department of Defense Instruction No. 4165.57.

Prohibited and Discouraged Uses: The applicant does not propose any prohibited or discouraged uses in Compatibility Zones B1-APZ-I and B1-APZ-II. Industrial warehouse buildings and repair services are compatible within Accident Potential Zones I and II pursuant to the 2018 Air Installation Compatible Use Zone (AICUZ) study disseminated by the United States Air Force. Use as an industrial warehouse is also compatible pursuant to Department of Defense Instruction (DoDI) No. 4165.57, but the intensity levels of this project in the absence of the Covenant would exceed DoDI allowances, as understood by the Air Force.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being within the 65-70 CNEL range. While the proposed warehouse/service repair facility is not a noise-sensitive use and would not require special measures to mitigate aircraft-generated noise, such measures may be required to achieve reduced interior noise levels of 45 dBA CNEL in office areas as required pursuant to the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan.

Part 77: The elevation of Runway 14-32 at its northerly terminus is approximately 1,535 feet above mean sea level (1,535 feet AMSL). At a distance of approximately 6,350 feet from the runway, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,598 feet AMSL. The project's site elevation is 1,540 feet AMSL. With a maximum building height of 25 feet, the top point elevation would be 1,565 feet AMSL. Therefore, review of this building by the FAA Obstruction Evaluation Service (FAA OES) is not required.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically. However, development within Compatibility Zone B1-APZ-I and APZ-II is limited to a maximum lot coverage of 50%. The maximum lot coverage proposed is 6%, which is consistent with the maximum lot coverage criterion.

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated (Section 2.3.2 of FAA Advisory Circular 5200-33C). The project is located 7,835 feet from the runway, and therefore would be subject to the above requirement.

Although the nearest portion of the proposed project is located within 10,000 feet of the runway (approximately 7,835 feet), the project utilizes underground detention systems which will not contain surface water or attract wildlife and, therefore, would not constitute a hazard to flight.

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight or circling climb following takeoff or toward an aircraft engaged in a straight or circling final approach toward a landing at an airport, other than a DoD or FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight or circling climb following takeoff or towards an aircraft engaged in a straight or circling final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, congregate care facilities, hotels/motels, places of assembly (including, but not limited to places of worship and theaters), buildings with more than 1 aboveground habitable floors, hazardous materials and critical community infrastructure facilities.
 - (f) Highly noise-sensitive outdoor non-residential uses. Examples of noise-sensitive outdoor nonresidential uses that are prohibited include, but are not limited to, major spectator-oriented sports stadiums, amphitheaters, concert halls, and drive-ins.
 - (g) Other hazards to flight.

3. Prior to issuance of any building permits, the landowner shall convey and have recorded an avigation easement to the March Inland Port Airport Authority or its successor in interested, or provide evidence that such easement has previously conveyed. The Airport Authority may waive this requirement in the event that the Authority determines that pre-existing avigation easements dedicated to the United States of America are sufficient to address its needs. Contact the March Joint Powers Authority at (951) 656-7000 for additional information.
4. The attached notice shall be given to all prospective purchasers and/or tenants of the property. While not required, the applicant and its successors-in-interest are encouraged to provide a copy of said notice to employees who would regularly be working at this location.
5. The project has been conditioned to utilize underground detention systems, which shall not contain surface water or attract wildlife. Any other proposed basin would require review and approval by the ALUC. Any new detention basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the detention basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

6. Office space must have sound attenuation features sufficient to reduce interior noise levels from exterior aviation-related sources to no more than CNEL 45 dB. March Joint Powers Authority shall require an acoustical study to ensure compliance with this requirement.
7. This project has been evaluated for **1,048 square feet of office area, 1,797 square feet of warehouse area, 2,830 square feet of storage area, 326 square feet of break room area, 420 square feet of lobby area, 192 square feet of conference room area, 433 square feet of lobby area, 1,995 square feet of office area, 1,644 square feet of warehouse area, 1,208 square feet of storage area, 348 square feet of break room area, 180 square feet of conference room area** 5 service repair bay doors, 1 wash bay door, and two truck fueling spaces. Any

increase in building area, change in use to any higher intensity use, change in building location, or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.

8. All buildings shall be designed with zoned fire sprinkler systems.
9. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
10. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission and March Air Reserve Base.
11. In order to ensure proper functioning of the project drain system to avoid potential hazards to March Air Reserve Base flights, an additional Best Management Practice (BMP) shall be added to the project Water Quality Management Plan (WQMP). The applicant shall enter into a covenant and agreement with the City of Riverside similar to the Water Quality Management Plan and Urban Runoff BMP Transfer, Access and Maintenance Agreement between March Joint Powers Authority and Sun Life Assurance Company of Canada (Document No. 2014-0030862), which shall be recorded prior to issuance of a certificate of occupancy. A copy of the recorded agreement and BMP shall be provided to the Riverside County Airport Land Use Commission. The BMP shall include the following program:
 - a. The property owner (Alessandro Property Investments LP or its successor(s)-in-interest, hereinafter "Owner") or its designated representative shall monitor the conditions of the detention basins and promptly inspect such basins following the completion of each "significant" rain event and the 48-hour period thereafter.
 - b. If any standing water remains in a basin that is not beneath a rock, gravel, or other layer following the completion of the "significant" rain event and the 48 hour period thereafter, Owner or its designated representative shall arrange to have such standing water either removed or covered within the next two business days following the conclusion of the 48 hour period.
 - c. In the event that the standing water situation recurs on a regular basis following the 48-hour detention period, the detention basin may no longer be draining as originally designed to prevent standing water from rising above a rock, gravel or other layer (for example, due to a rise in groundwater levels or other circumstance beyond Owner's ability to control). In that situation, Owner or its designated representative shall promptly engage a licensed civil engineer to prepare a design plan to assure that such

condition does not persist for more than 48 hours following the conclusion of a “significant” rain event. The required engineering design solution shall be implemented promptly, but no later than 180 days following its approval by all applicable authorities, providing that, until such time as the engineered design solution is implemented, Owner or its designated representative will maintain water levels below the rock, gravel, or other layer.

12. The applicant has agreed to accept a Covenant which will be recorded on the title of the property restricting the actual occupancy of the buildings to the limits of the Air Force Instruction. The project shall be in compliance with the recorded and executed Covenant, which limits building occupancy to a maximum of 25 people in any given acre in APZ-I, and a maximum of 50 people in any given acre in APZ-II. The Covenant shall include the following language:

“Covenanter has agreed to comply with the Density Restrictions and a Density Cap (both terms are defined below), by limiting occupancy of the Project to (i) twelve (12) occupants (“Density Cap”) [**THE DENSITY CAP WILL DECREASE IF THE SQUARE FOOTAGE OF THE BUILDING DECREASES.**]; (ii) twenty-five (25) occupants in any square area measuring 208 feet by 208 feet (“Square Area”) for all Square Areas within portions of the building of the Project within APZ I, and (iii) fifty (50) occupants in any Square Area within portions of the building of the Project within APZ II. Requirement (ii) and (iii) are collectively the “Density Restrictions”, and are depicted in Exhibit B, attached hereto and incorporated herein by reference. Accordingly, any building expansion is prohibited, including an increase in building area, without further review by the City and MARB representatives, and consent and approval provided through an amendment to this covenant.”

Compliance shall be verified by City or third-party inspections and reports on a schedule agreed upon by the applicant/project operator, the City, and MARB representatives.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

**THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS**

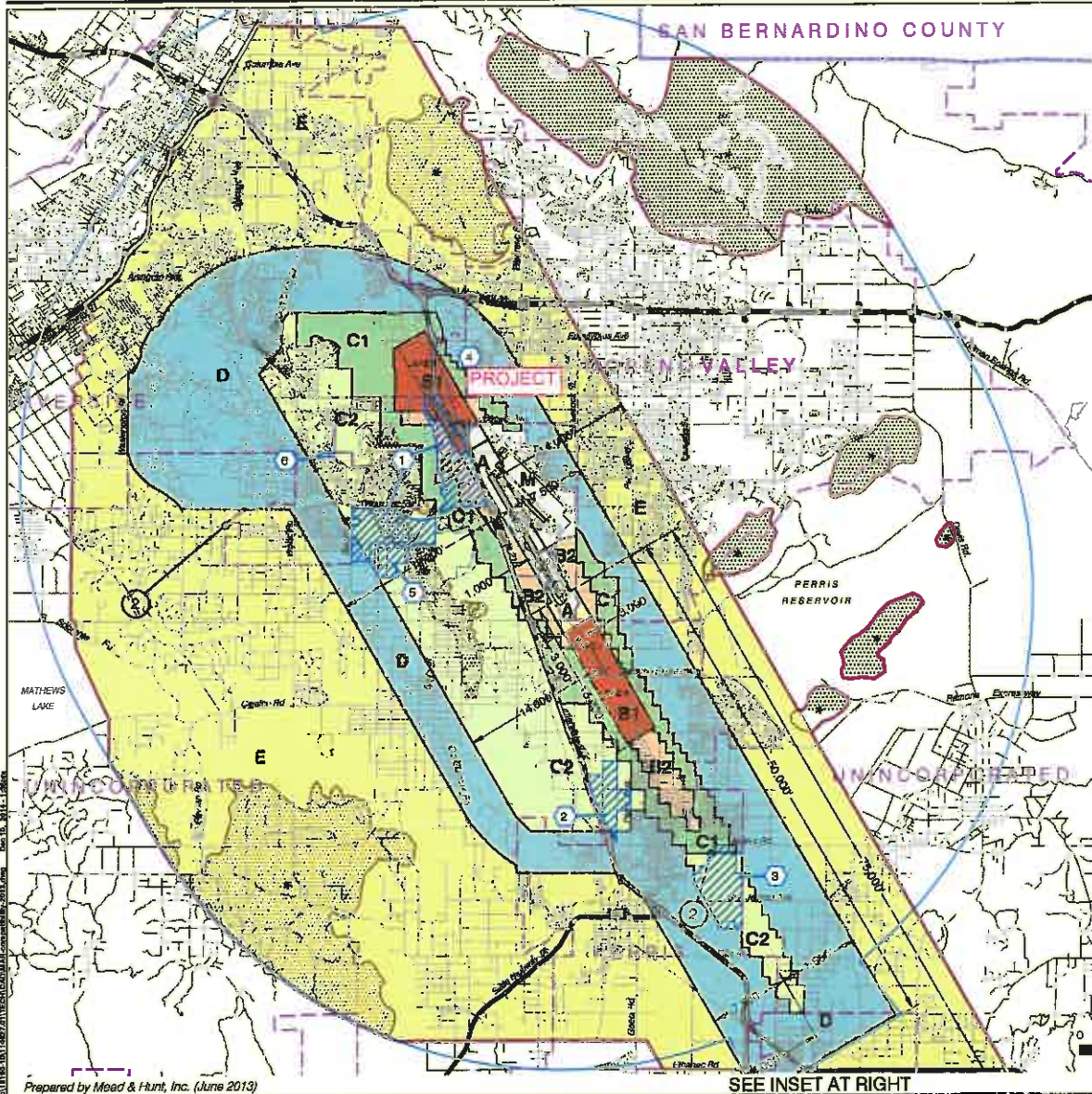
**PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES**



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



LEGEND

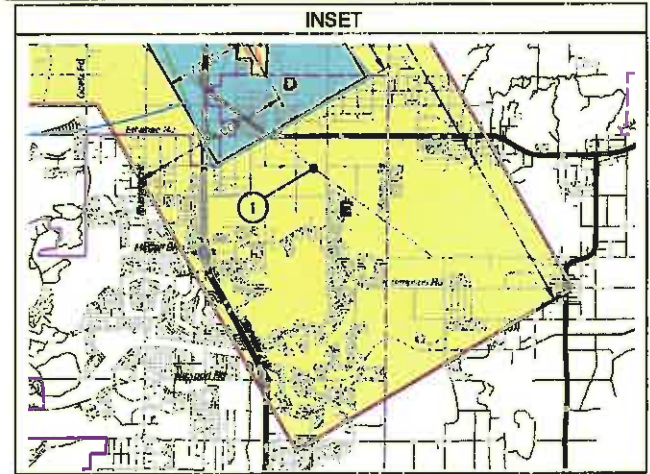
- Compatibility Zones**
- Airport Influence Area Boundary
 - Zone A
 - Zone B1
 - Zone B2
 - Zone C1
 - Zone C2
 - Zone D
 - Zone E
 - Zone M
 - High Terrain Zone
 - FAR Part 77 Military Outer Horizontal Surface Limits
 - FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision



Riverside County
 Airport Land Use Commission
**March Air Reserve Base / Inland Port Airport
 Land Use Compatibility Plan**
 (Adopted November 13, 2014)

Map MA-1

Compatibility Map
 March Air Reserve Base / Inland Port Airport

Note:
 All dimensions are measured from runway ends and centerlines.

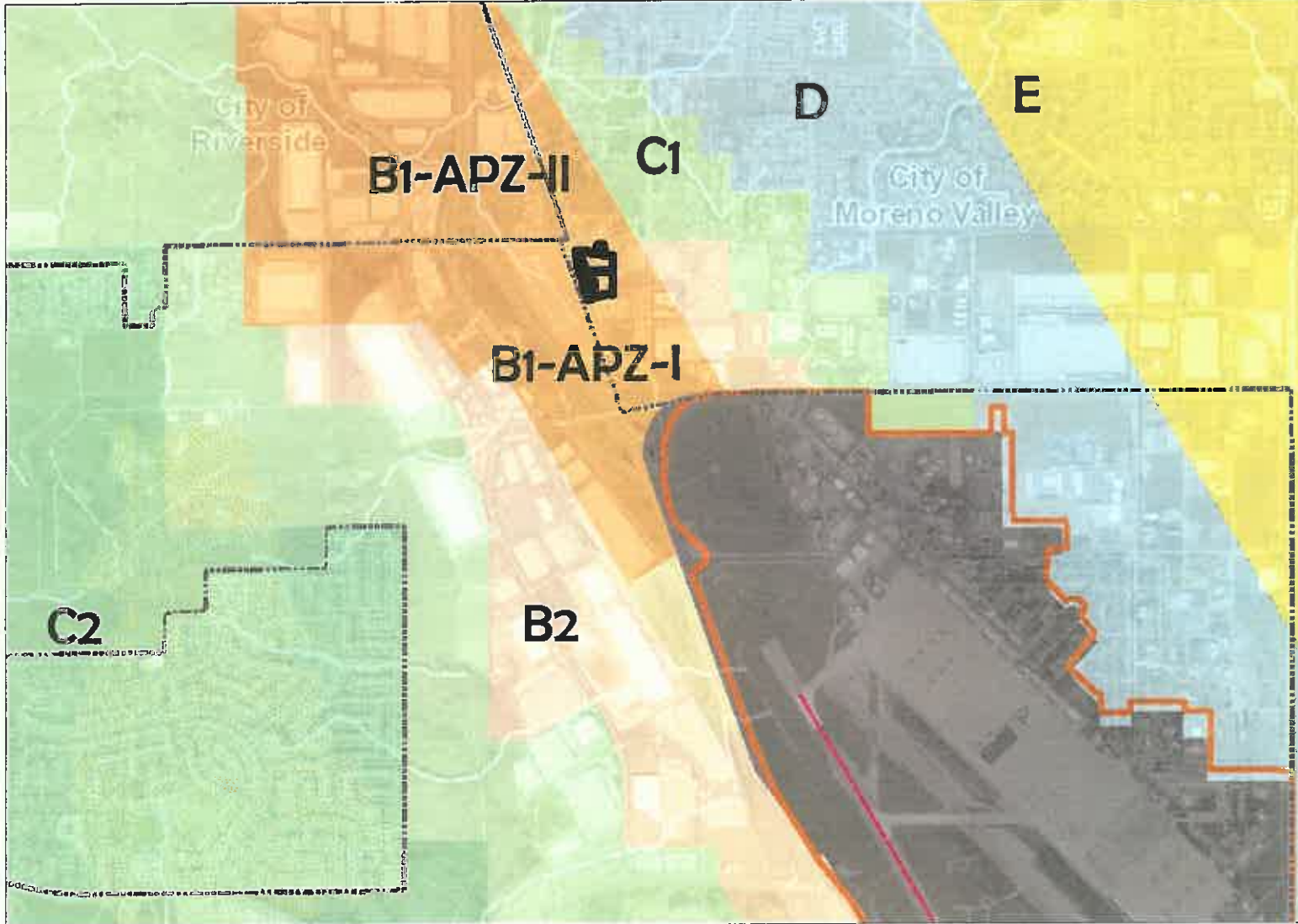


0 2 4 MILES

Base map source: County of Riverside 2013

SEE INSET AT RIGHT

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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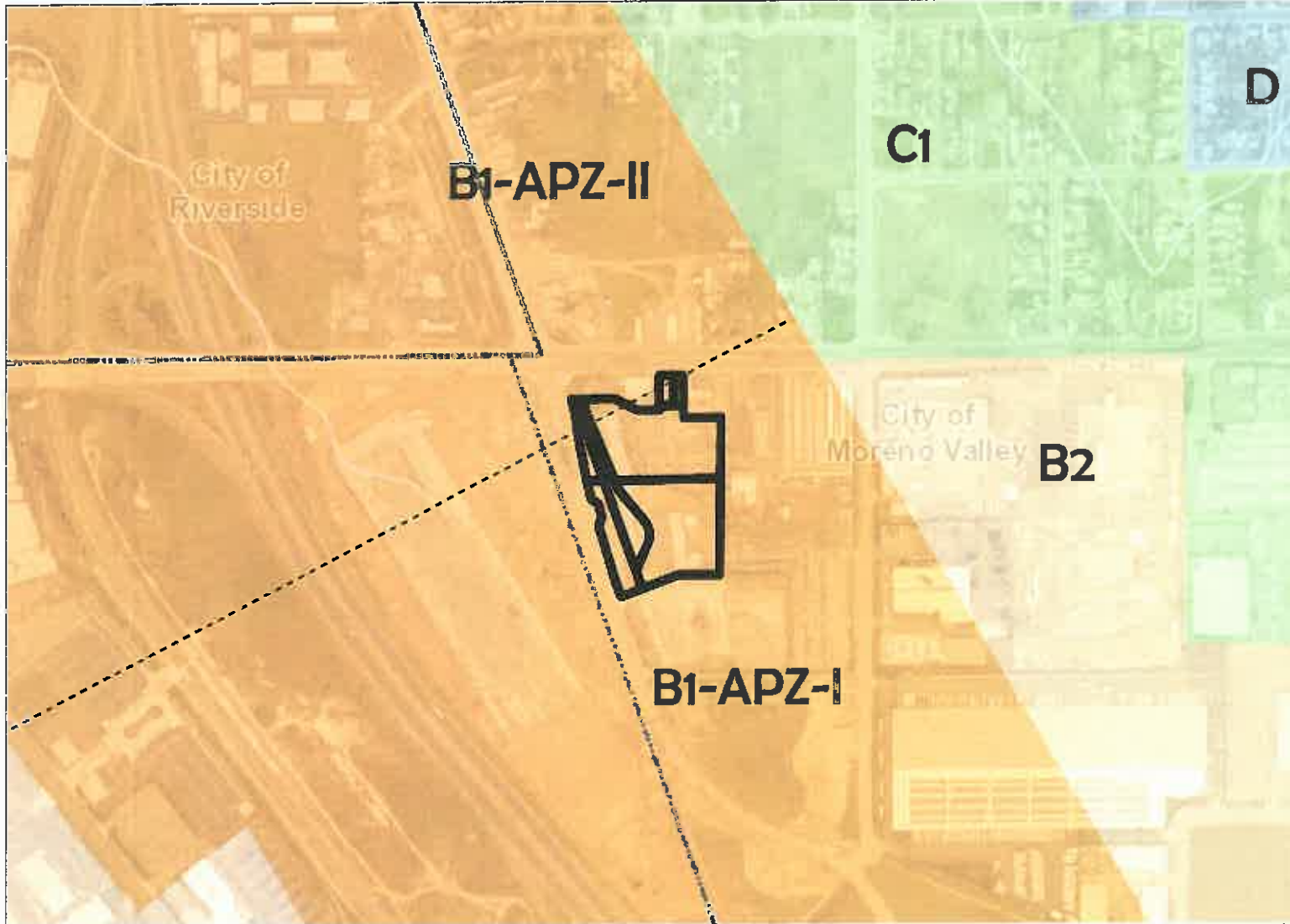


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Notes

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC8



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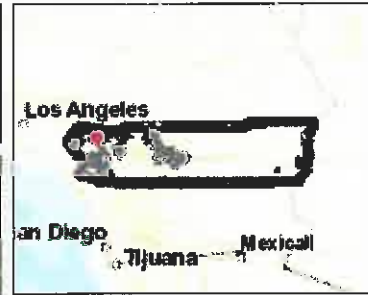
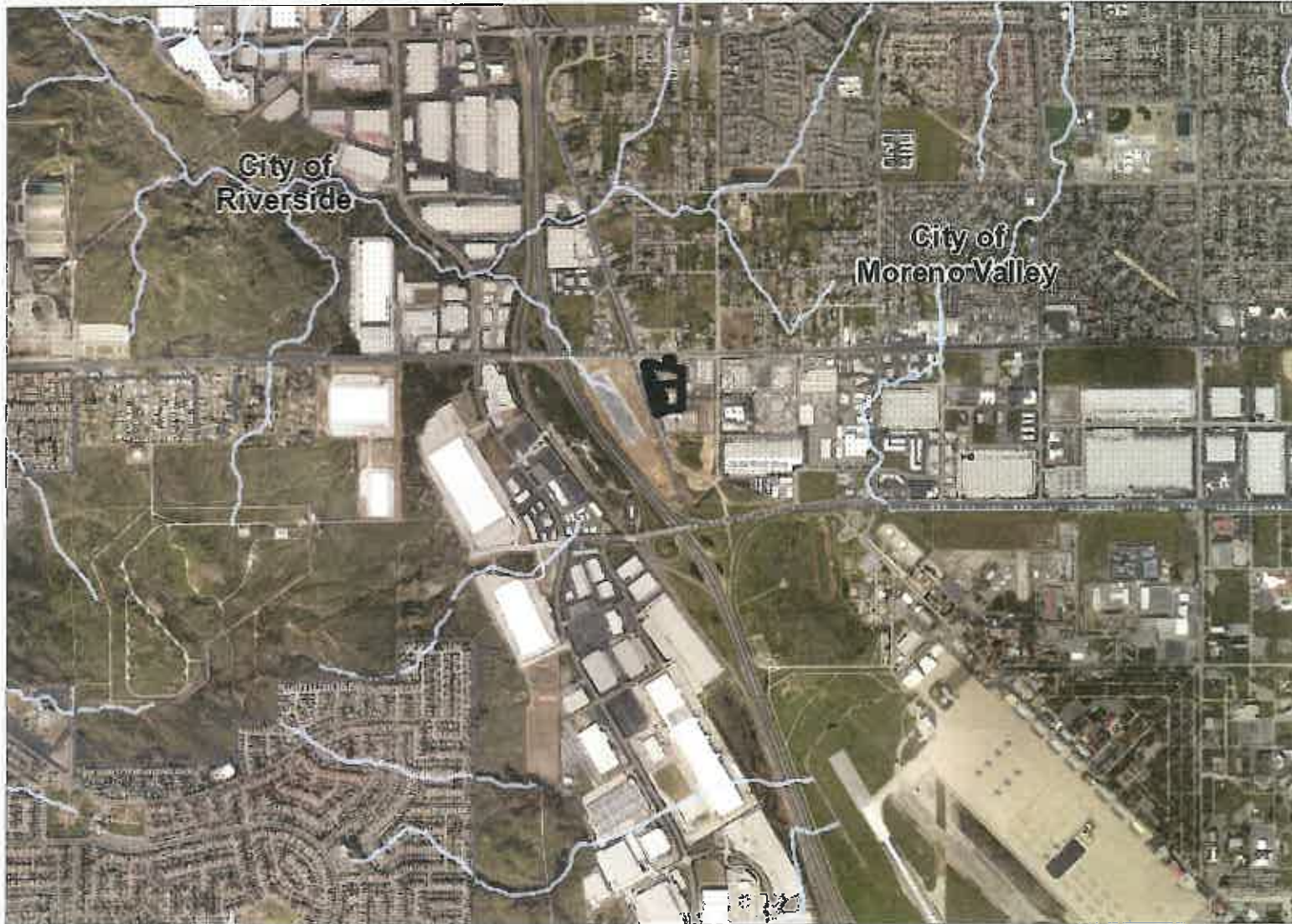


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Notes

Map My County Map



Legend

- Blue line symbol: Blueline Streams
- Grid symbol: City Areas
- Street symbol: World Street Map

Notes



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Map My County Map



Legend

- Blue line: Blueline Streams
- Black outline: City Areas
- Grey grid: World Street Map

Notes



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0 770 1,539 Feet

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**DEPARTMENT OF THE AIR FORCE
AIR FORCE RESERVE COMMAND**

April 6, 2021

Colonel Rodney E. McCraine
Commander
452d Mission Support Group
1261 Graeber Street, Bldg. 2313, Suite 135
March ARB, CA 92518

Mr. Paul Rull
Director
Riverside County Airport Land Use Commission
4080 Lemon Street, 14th Floor
Riverside, CA 92501

Dear Mr. Paul Rull,

This is in response to three applications, listed below, being considered by the Riverside County Airport Land Use Commission (ALUC) and addresses the application of Department of Defense Instruction (DODI) 4165.57 and Office Density Restriction Covenants for Riverside County Airport Land Use Commission (ALUC) on these cases –

- ZAP1444MA20 (Amazon Parcel Delivery Hub at Freeway Business Center, March JPA),
- ZAP1452MA21 (OZI Warehouse, City of Riverside),
- ZAP1456MA21 (Penske Truck Facility, City of Moreno Valley)

With respect to the 3 projects, and for the any future projects and applications, March ARB encourages the ALUC to follow the DODI 4165.57 as a minimum guide to determining office density restrictions and appropriate covenants to be used in ensuring that the density restrictions are met. The Instruction encourages local governments to address labor intensity, which addresses the allowable number of persons within a building and even in portions of the buildings. The most recent F-16 aircraft accident in March 2019 near March ARB is an example of the risk to property and life that exists when developments encroach upon an Air Base. Luckily, the F-16 crashed in a portion of the building with very low labor intensity and no one was injured. However, the accident could have easily happened only a few feet to the west where the higher labor intensity of the office space in that building could have resulted in serious bodily injury or death.

The ALUC should always consider the risk of an aircraft accident when making a decision. The use of Accident Potential Zones (APZ) described in the DODI 4165.57 address the general risk, but the DODI is only minimal guidance for use by local authorities to reduce the risk to the general public. There are two equally important aspects to the risk: the potential for aircraft to have an accident, and the potential for the public to be impacted by an accident. The Air Force (AF) is ready to work with ALUC to address March ARB risks and approaches to reduce the potential for accidental injury or death to the public both in and outside of allowable developments within the APZs. It is the Air Forces desire that the ALUC would side with conservative limits on building and development occupations.

The ALUC and local governments can do much to reduce potential accidents by having strong codes to reduce the cumulative development impact on air-pollution, electronic interference with aircraft, height of structures, and potential glare to pilots. Likewise, ALUC and local governments can greatly reduce the potential risk to the public by strong codes to reduce occupation density of land uses within the APZs, not only within certain land uses, but also labor intensity within building uses in the APZs.

ALUC's use of deed covenants to restrict building occupation in developments, which are not located in a city with adequate land use controls, is encouraged by the AF. The specific wording must ensure that the entity having jurisdiction be able to enforce the covenant. The AF is not able to inspect or enforce any deed covenants. This is a local government responsibility and authority. Covenants are not easily enforced and strict land use controls by the City and County are the real solution to not only reduce the risk but to enforce the reduced risk in the future.

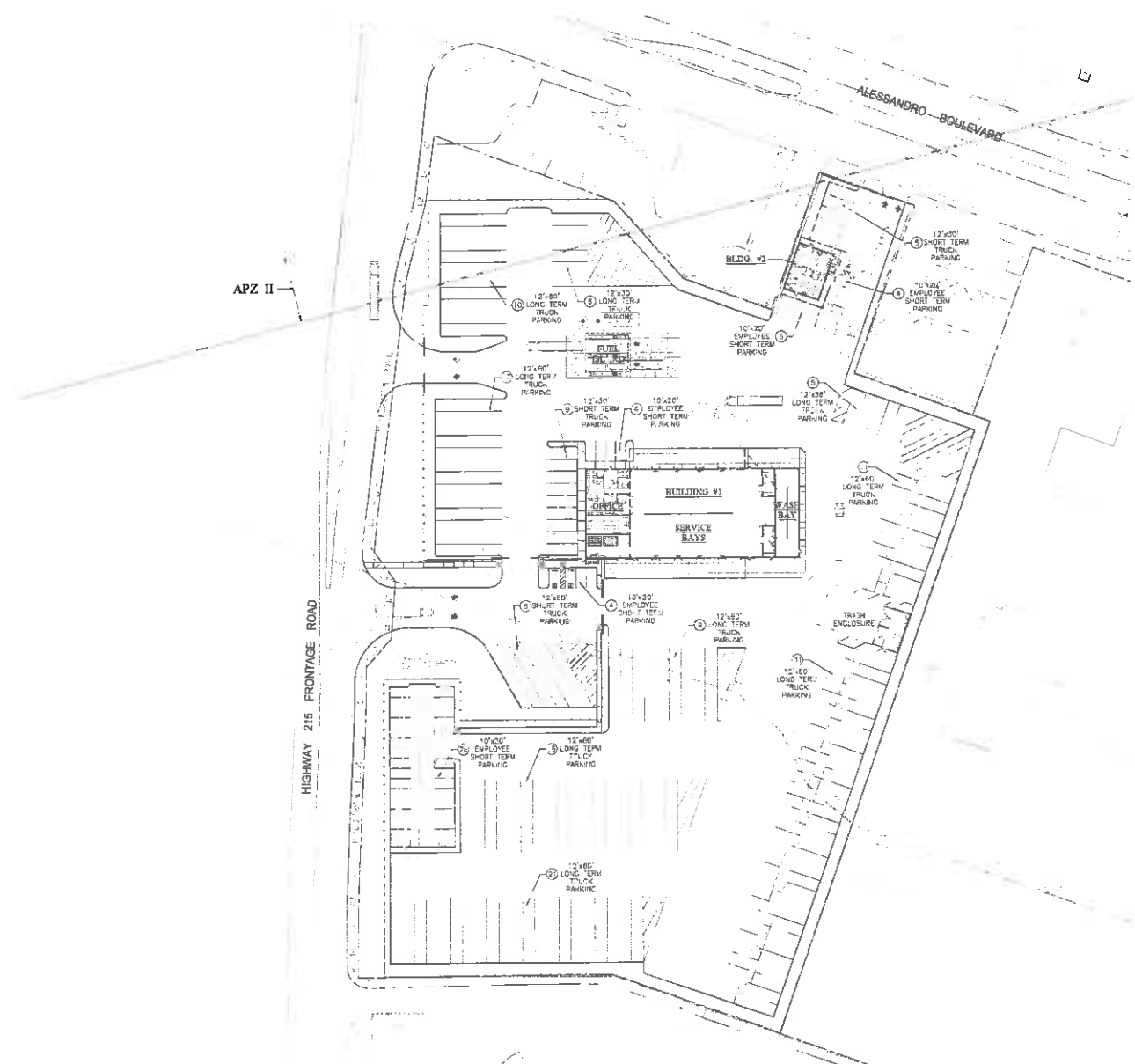
Finally the AF will work with the ALUC in its Compatibility Use Study to develop a Compatibility Use Plan to develop further guidance for local governments to address and enforce; occupation density in APZs, labor intensity within buildings and other factors, to include the cumulative impacts of multiple building solar glare, air emissions, bird and wildlife hazards, etc. We look forward to working with the ALUC and its member governments in this regard.

Should you have any questions or would like to discuss this further, please feel free to contact me directly at (951) 655-3700.

Sincerely

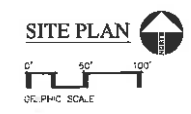
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MCCRAINE.RODNEY.ERI
DNEY.ERIC.10 C.1067944492
67944492 Date: 2021.04.05
15:55:27 -07'00'

RODNEY E. MCCRAINE, Colonel, USAF
Commander, 452d Mission Support Group



PARKING DATA

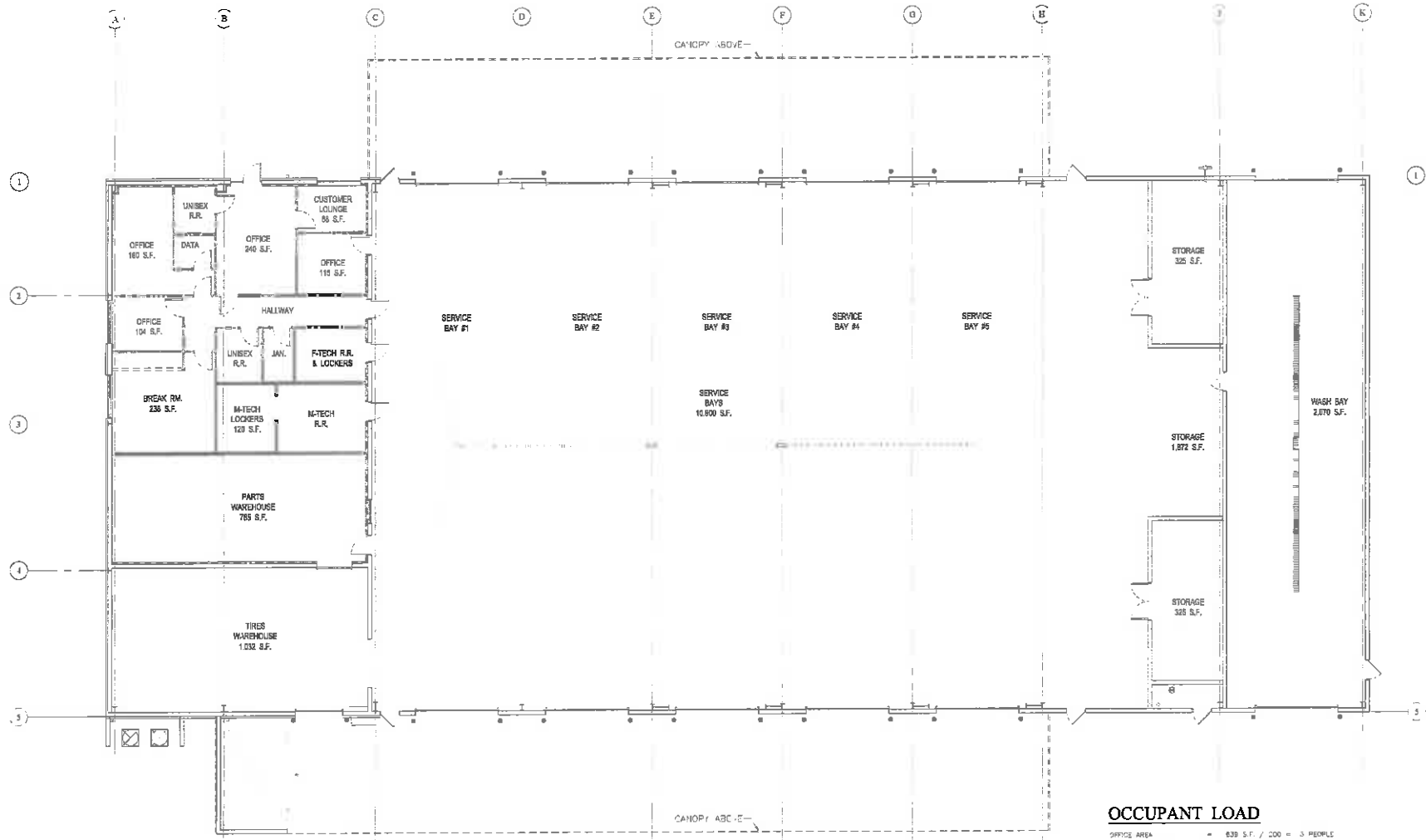
12' x 60' LONG TERM TRUCK PARKING	= 13 SPACES
12' x 60' SHORT TERM TRUCK PARKING	= 11 SPACES
12' x 20' SHORT TERM TRUCK PARKING	= 18 SPACES x 1.0 = 18 PEOPLE
12' x 60' SHORT TERM TRUCK PARKING	= 6 SPACES x 1.0 = 6 PEOPLE
10' x 20' SHORT TERM CAR PARKING	= 44 SPACES x 1.5 = 66 PEOPLE



NEW FACILITY
 FOR
PENSKE TRUCK LEASING
 HWY. 215 FRONTAGE ROAD
 MORENO VALLEY, CA.

K/G
 ARCHITECTS
 7315 E. REDFIELD ROAD
 SUITE 302
 SCOTTSDALE, AZ 85260
 (480) 943-0705 - TEL.
 (480) 443-3185 - FAX

DR. IN CH.	DATE	REVISIONS	D. DATE	ISSUED FOR:
BIA			03-31-21	REVIEW
CHECKED BY:				SHEET NO.
DATE				A1-0
PROJECT NO.	SITE PLAN			
27104				



FLOOR PLAN



OCCUPANT LOAD

OFFICE AREA	=	839 S.F. / 200	=	3 PEOPLE
RECEPTION AREA	=	1,767 S.F. / 500	=	3 PEOPLE
STORAGE AREA	=	3,064 S.F. / 300	=	9 PEOPLE
BREAK ROOMS	=	336 S.F. / 15	=	22 PEOPLE
5 SERV. BAYS + WASH	=	(6) x 1.5	=	9 PEOPLE
FUEL BAY	=	(2) LANES x 1.5	=	3 PEOPLE
			=	49 PEOPLE TOTAL



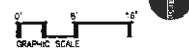
NEW FACILITY
FOR
PENSKE TRUCK LEASING
HWY 215 FRONTAGE ROAD
MORENO VALLEY, CA.

K/G
ARCHITECTS
7555 E. REDFIELD ROAD
SUITE 102
SCOTTSDALE, AZ 85260
(480) 443-3785 - TEL.
(480) 443-3185 - FAX

DR./IN BY:	DATE:	REVISIONS:	DATE:	DESIGNED FOR:
BA:			03-31-21	REVIEW
CHECKED BY:			SHEET NO:	
LA, PE				
PROJECT NO:	BUILDING #1 OCCUPANCY PLAN			
21104				A2-0



FLOOR PLAN



OCCUPANT LOAD

RENTAL LOBBY	=	420 S.F. / 15	=	28 PEOPLE
OFFICE AREA	=	405 S.F. / 200	=	2 PEOPLE
STORAGE AREA	=	386 S.F. / 300	=	1 PEOPLE
CONFERENCE ROOM	=	182 S.F. / 15	=	13 PEOPLE
				= 44 PEOPLE

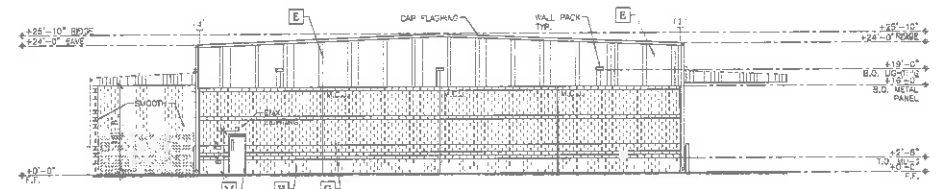


NEW FACILITY
FOR
PENSKE TRUCK LEASING
HWY. 215 FRONTAGE ROAD
MORENO VALLEY, CA.

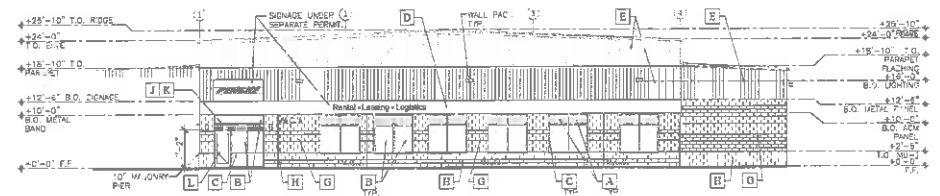
K/G
ARCHITECTS
7565 E. REDFIELD ROAD
SUITE 102
SCOTTSDALE, AZ 85266
(480) 943-3785 - 238
(480) 943-3785 - FAX

DRAWN BY:	DATE:	PER/SOFT:	DATE:	ISSUED FOR:
SHA			03-31-21	REVIEW:
CHECKED BY:			SHEET NO:	
DATE:				
PROJECT NO:	BUILDING #2 OCCUPANCY PLAN		A2-1	
21/04				

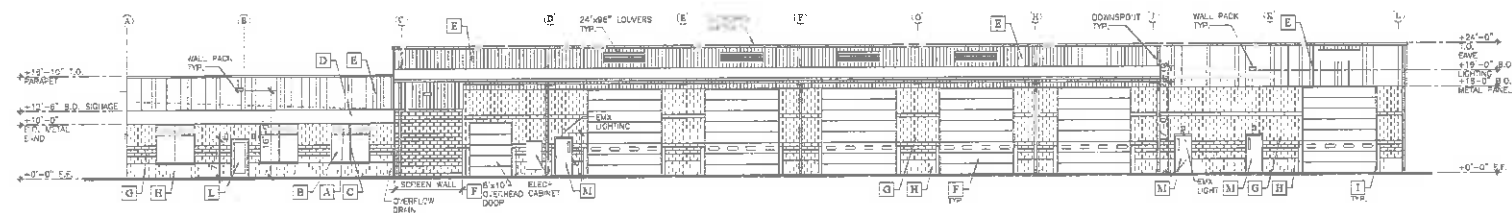
EXTERIOR MATERIALS & FINISH SCHEDULE			
MARK	MATERIAL	LOCATION	DESCRIPTION
A	GL-2 BRUNDEL CLASS	ABOVE SFT WHERE NOTED AT EXTERIOR WINDOWS	1" INSULATED SPANDREL GLAZING, COLOR: BLACK OPaque - TEmPERED AS REQUIRED
B	GL-1 VELUM CLASS	TYPICAL AT EXTERIOR DOORS AND WINDOWS AS NOTED	1" INSULATED GLAZING, COLOR: LIGHT GRAY/INT WITH LC-9 'E' COATING ON #3 SURFACE - TEmPERED AS REQUIRED
C	SF-1 STOREFRONT FRAMING	STOREFRONT DOOR & WINDOW LOCATIONS	CLEAR ANODIZED ALUMINUM STOREFRONT FRAMING
D	MP-1 METAL PANEL (YELLOW)	RENTAL SERVICE & OFFICE ADDRESS BUILDING BANDING	A.C.M. METAL PANEL; ACM COLOR: YELLOW
E	MP-2 METAL PANEL (BROWN)	AND METAL FACADE PANELS, DOWNSPOUTS & TRIM	A.P. METAL BUILDING PANELS, DOWNSPOUTS AND TRIM COLOR: MEDIUM BRONZE
F	D-3 INSULATED OVERHEAD DOOR	SERVICE AND WASH BAYS	OVERHEAD DOOR, SERIES 392 - EXTERIOR PAINT COLOR: ARCHITECTURAL BROWN
G	UM-2 MASONRY UNIT	BUILDING FACADE ABOVE 2'-8"	8" x 8" x 16" STANDARD GRAY SPLIT FACE CMU
H	UM-1 MASONRY UNIT	BUILDING BASE TO 2'-8"	UP TO 2'-8" A.F.F., SPLIT FACE MASONRY WITH ELASTOMERIC PAINT COLOR EXP-11
I	BC-1 BOLLARD COVER	SERVICE BUILDING EXTERIOR AT DOORS AND UTILITIES	STEEL PIPE BOLLARD; WITH SLEEVE COVERS
J	PC-2 ALUMINUM BAND	ABOVE STOREFRONT ENTRANCES	ANODIZED ALUMINUM BAND COLOR: KEDEEN BLUE (UPPER BAND)
K	PC-1 ALUMINUM BAND	ABOVE STOREFRONT ENTRANCES	POWDER COATED ALUMINUM BAND COLOR: ULTRAWHITE BLUE (UPPER BAND)
L	D-1 ALUMINUM STOREFRONT DOOR	ENTRY DOOR, RENTAL, OFFICE & SERVICE ENTRANCE	FULL VISION DOOR
M	D-2 HOLLOW METAL DOOR	RENTAL STORAGE / SHOP ENTRY / WASH BAY	PRE-FINISHED ARCH BROWN COLOR EXP-3



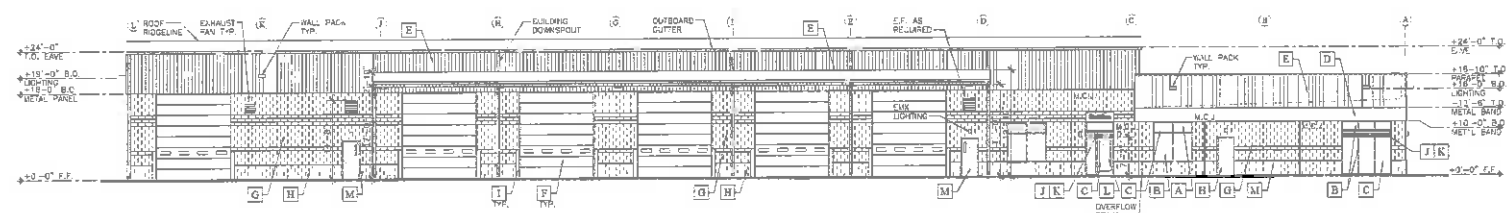
1 EAST ELEVATION
SCALE: 3/32" = 1'-0"



2 WEST ELEVATION
SCALE: 3/32" = 1'-0"



3 SOUTH ELEVATION
SCALE: 3/32" = 1'-0"



4 NORTH ELEVATION
SCALE: 3/32" = 1'-0"



NEW FACILITY
FOR
PENSKE TRUCK LEASING
HWY. 215 FRONTAGE ROAD
MORENO VALLEY, CA.

K/G
ARCHITECTS
758 E REDFIELD ROAD
SUITE 102
SCOTTSDALE, AZ 85260
(480) 443-3788 - TEL.
(480) 443-3888 - FAX

DESIGNED BY:	DATE:	REVISION:	DATE:	ISSUED FOR:
CHECKED BY:			02-18-21	REVIEW
PROJECT NO:	ELEVATIONS			SHEET NO:
21104				A7-0



Penske Truck Leasing
1711 W. Greentree Dr.
Suite 117
Tempe, AZ 85284

Tel: 602-253-0041
Fax: 480-7754350

www.pensketruckleasing.com

March 25, 2021

Riverside County Airport Land Use Commission
40480 Lemon Street., 14th. Floor
Riverside, CA 92501
Attn: Paul Rull, ALUC Principal Planner

RE: ZAP1456MA21 ALUC Review

Dear Mr Rull,

Penske Truck Leasing would like to change the design of the building and site plan to accommodate two separate buildings from the initial set that was submitted on March 16. I will be emailing a copy of the revised plans (site, building and calculations) along with a hard copy overnighted to your office over the next day for you to begin your preliminary intensity analysis of the new layout.

Sincerely,

A handwritten signature in black ink, appearing to read "Mike Barnes".

Mike Barnes
Penske Truck Leasing Co.
Senior Regional Facilities Manager
480-276-5888



Penske Truck Leasing
1711 W. Greentree Dr.
Suite 117
Tempe, AZ 85284

Tel: 602-253-0041
Fax: 480-7754350

www.pensketruckleasing.com

March 16, 2021

Riverside County Airport Land Use Commission
40480 Lemon Street., 14th. Floor
Riverside, CA 92501
Attn: Paul Rull, ALUC Principal Planner

RE: ZAP1456MA21 ALUC Review

Dear Mr Rull,

The Site Plan intended for this location includes Short Term and Long Term parking. Short Term is described as parking for everyday use by customers, employees (shown in Grey on the plan). These spaces turn over daily. Long Term parking is described as trucks and trailers parked in the stall at or greater than 10 days. This involves trucks and trailer dropped off by the customer and parked, waiting for servicing in a time frame of 10-15 days before it get picked up. This tends to be 90% of our operation while 10% for that of Short Term.

Sincerely,

A handwritten signature in black ink, appearing to read "Mike Barnes", written over a faint horizontal line.

Mike Barnes
Penske Truck Leasing Co.
Senior Regional Facilities Manager
480-276-5888



Project Narrative Moreno Valley, CA February 22, 2021

Penske Truck Leasing Co, L.P. ("Penske") is proposing to construct a new State-of-the-Art (6)-Bay Service Facility (w/7,080 sq.ft. office core and Wash Bay) and associated (2)-Lane, (3)-product (gas, diesel, DEF) Fuel Island located in the Moreno Valley, CA vicinity.

Penske's intended use of the premises is for "the operation of a motor vehicle and truck leasing, rental and sales business, including the housing, maintaining to full capacity and repairing of motor trucks and trailers, outside parking and storage of such vehicles, motor vehicle repair shop and the storage and dispensing of fuel for the benefit of our internal customers only".

We conduct four essential functions from the facility: (1) Used Truck Sales, (2) Provide Local One-way rentals to the General Public and Commercial Industry, (3) Provide Full Service Lease (FSL) or Contract Maintenance (CM) to our contractual customers and (4) Provide maintenance, fueling and washing of our supporting fleet (*Truck Sales is optional at some locations and may not be applicable*).

Penske will conduct truck and trailer repairs consisting of general and preventative maintenance to our private and customer fleet (not open to the general public) such as clutches, oil changes, belt/bulb replacements, tune-ups, tire changes, etc. No major work such as framework, collision repair or Body Shop work, etc. would occur. All work is conducted within the facility.

A high-level overview of our maintenance operation/product lines for our location:

- Full Service Lease (FSL) - 4, 5, and 6 year leasing of commercial transportation equipment
Equipment Type - Semi tractors | Box Trucks | Semi Trailers.
The customer rate for this equipment guarantees all of the maintenance on these units. The customer understands what the monthly costs are in one essential rate. We assume all of the maintenance expense for these units. Penske owns these Assets / we sell the units at the end of the lease agreement.
- Contract Maintenance (CM) - Customer owns the unit and agrees to a set term (years of term vary).
Penske provides the maintenance at a contractually agreed upon labor and parts rate.
- Washing and Fueling - Only Penske Vehicles and/or Customers can Wash or fuel on the premises. The location is closed to the general public for these services.

Location Information:

Penske typically operates 2 shifts, subject to change:

Service: Monday – Friday 6:00 AM to 12 AM
Saturday 7:00 AM to 4:00 PM
Sunday 7:00 AM to 12:00 PM

Rental: Mon – Fri 7:00 AM to 6:00 PM
Sat 7:00 AM – 4:00 pm
Sun 7:00 AM to Noon



- We anticipate employment of approximately 31 associates at this location:
 - 4 - Office staff (rental, leasing, sales)
 - 20- Fully certified diesel technicians
 - 7 - Service staff
- Total Associates at Facility: 31

Position	Day Shift Count (6am-3pm)	2nd Shift Count(3pm-12am)
Tech 3	3	4
Tech 2	4	5
Tech 1	2	2
CSR	1	1
Maint Supervisor	1	1
BSM	1	
Service Admin Clerk	1	
Parts Clerk	1	
Branch Admin Clerk	1	
BRM	1	
RMT	2	
Total	18	13

NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893**. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The City of Moreno Valley Planning Department should be contacted on non-ALUC issues. For more information please contact City of Moreno Valley Planner Mr. Jeff Bradshaw at (951) 413-3224.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to prull@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please telephone Barbara Santos at (951) 955-5132.

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside California

DATE OF HEARING: May 13, 2021

TIME OF HEARING: 9:30 A.M.

Pursuant to Executive Order N-25-20, this meeting will be conducted by teleconference and at the Place of Hearing, as listed above. Public access to the meeting location will be allowed, but limited to comply with the Executive Order. Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org

CASE DESCRIPTION:

ZAP1456MA21 – Penske Truck Leasing Co., L.P/CD Moreno Holding LLC. – City of Moreno Valley Case No. PEN21-0025 (Plot Plan) a proposal to construct a Penske Truck facility including a 19,200 square foot building, a 1,792 square foot building, and a 1,936 square foot truck fueling island on 9.10 acres located southerly of Alessandro Boulevard, easterly of Old 215 Frontage Road, and northerly of Cactus Avenue. Originally, a 16 building industrial business park (located partially on the proposed site) was proposed (ZAP1038MA7) and found consistent by the Commission in 2007. A revision to the project was proposed (ZAP1333MA18) that included 10 industrial warehouse buildings and a 15,280 square foot Penske Truck facility building, was found consistent by the Commission in 2018 (Airport Compatibility Zones B1-APZ-I and B1-APZ-II of the March Air Reserve Base/Inland Port Airport Influence Area).



RIVERSIDE COUNTY

AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP1456MAZ1 DATE SUBMITTED: 02/24/2021

APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

Applicant	PENSKE TRUCK LEASING CO, L.P.	Phone Number	602-253-0041
Mailing Address	2675 MORGANTOWN ROAD	Email	MIKE.BARNES@PENSKE.COM
	READING, PA 19607		

Representative	MIKE BARNES, PENSKE TRUCK LEASING CO, L.P.	Phone Number	480-276-5888
Mailing Address	1711 W. GREENTREE DR. SUITE 117	Email	MIKE.BARNES@PENSKE.COM
	TEMPE, ARIZONA 85284		

Property Owner	CD MORENO HOLDING LLC. ATTN: TOM K KOSS	Phone Number	949-697-2335
Mailing Address	30782 HUNT CLUB DRIVE	Email	TOMKKOSS@GMAIL.COM
	SAN JUAN CAPISTRANIO, CA 92675		

LOCAL JURISDICTION AGENCY

Local Agency Name	CITY OF MORENO VALLEY	Phone Number	951-413-3224
Staff Contact	JEFFREY BRADSHAW	Email	JEFFREYB@MOVAL.COM
Mailing Address	14117 FREDERICK STREET	Case Type	
	MORENO VALLEY, CA 92553	<input type="checkbox"/> General Plan / Specific Plan Amendment	
		<input type="checkbox"/> Zoning Ordinance Amendment	
		<input type="checkbox"/> Subdivision Parcel Map / Tentative Tract	
		<input type="checkbox"/> Use Permit	
Local Agency Project No	<u>PEN21-0025</u>	<input checked="" type="checkbox"/> Site Plan Review/Plot Plan	
		<input type="checkbox"/> Other	

PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address	HIGHWAY 215 FRONTAGE ROAD	Gross Parcel Size	9.102 ACRES
Assessor's Parcel No.	297-120-002, 297-120-003, PORTION OF 297-100-088, 297-100-078, 297-100-078, 297-120-016, 297-120-017, 297-120-018	Nearest Airport and distance from Airport	MARCH AIR RESERVE BASE 8,830 FEET
Subdivision Name	ALESANDRO TRACT		
Lot Number	3		

PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe) UNDEVELOPED PROPERTY WITHOUT EXSTING STRUCTURES

11/17/21
01-102-11

Proposed Land Use (describe)	PENSKE TRUCK LEASING PROPOSES TO CONSTRUCT A 21,540 SQUARE FOOT TRUCK LEASING FACILITY WITH A COVERED TWO LANE FUEL DISPENSING AND STORAGE AREA SITE WILL BE PAVED FOR TRUCK STORAGE AND CIRCULATION		
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)	1	
For Other Land Uses (See Appendix C)	Hours of Operation	M-F 6AM -6PM; Sat 7AM -4PM; Sun 7AM - 12PM	
	Number of People on Site	20	Maximum Number 31 (Total Associates Employeed)
	Method of Calculation	Calculating shift count between day and 2nd. shift. People count will not exceed 20. (Proj Narrative breaks down people count)	
Height Data	Site Elevation (above mean sea level)	1,540	ft.
	Height of buildings or structures (from the ground)	26	ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No
	If yes, describe	_____	

- A. **NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. **REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. **SUBMISSION PACKAGE:**
1. Completed ALUC Application Form
 1. ALUC fee payment
 1. Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
 1. Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
 1. CD with digital files of the plans (pdf)
 1. Vicinity Map (8.5x11)
 1. Detailed project description
 1. Local jurisdiction project transmittal
 3. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
 3. Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. (Only required if the project is scheduled for a public hearing Commission meeting)

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.1

HEARING DATE: May 13, 2021

CASE NUMBER: ZAP1111FV21 – AZIZ, LLC (Representative: Michael Ramirez)

APPROVING JURISDICTION: County of Riverside

JURISDICTION CASE NO: CZ2100009 (Change of Zone)

LAND USE PLAN: 2007 French Valley Airport Land Use Compatibility Plan, as amended in 2011

Airport Influence Area: French Valley Airport

Land Use Policy: Airport Compatibility Zone E

Noise Levels: Outside 55 CNEL contour

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends that the proposed Change of Zone be found **CONSISTENT** with the 2007 French Valley Airport Land Use Compatibility Plan, as amended in 2011.

PROJECT DESCRIPTION: A proposal to change the zoning of 2.94 gross acres from Industrial Park (I-P) to Manufacturing Service Commercial (M-SC).

A previous proposal to construct a retail and fueling facility on this site was found consistent under ZAP1096FV20 as a staff review.

PROJECT LOCATION: The proposed project is located on the northwest corner of Jean Nicholas Road and Winchester Road in the unincorporated community of French Valley, approximately 10,990 feet northeasterly of the northern terminus of Runway 18-36 at French Valley Airport.

BACKGROUND:

Residential Density/Non-Residential Intensity: The site is located within Zone E of the French Valley Airport Influence Area, where residential density and non-residential intensity are not restricted.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zone E (hazards to flight).

Noise: The site is located outside the 55 CNEL aircraft noise contour for French Valley Airport. Therefore, no special measures to mitigate aircraft noise are required at this location.

Part 77: The elevation of Runway 18-36 at its northerly terminus is 1,347 feet above mean sea level (AMSL). At a distance of approximately 10,990 feet from the runway to the site, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,456 feet AMSL. The proposed change of zone application does not propose any buildings.

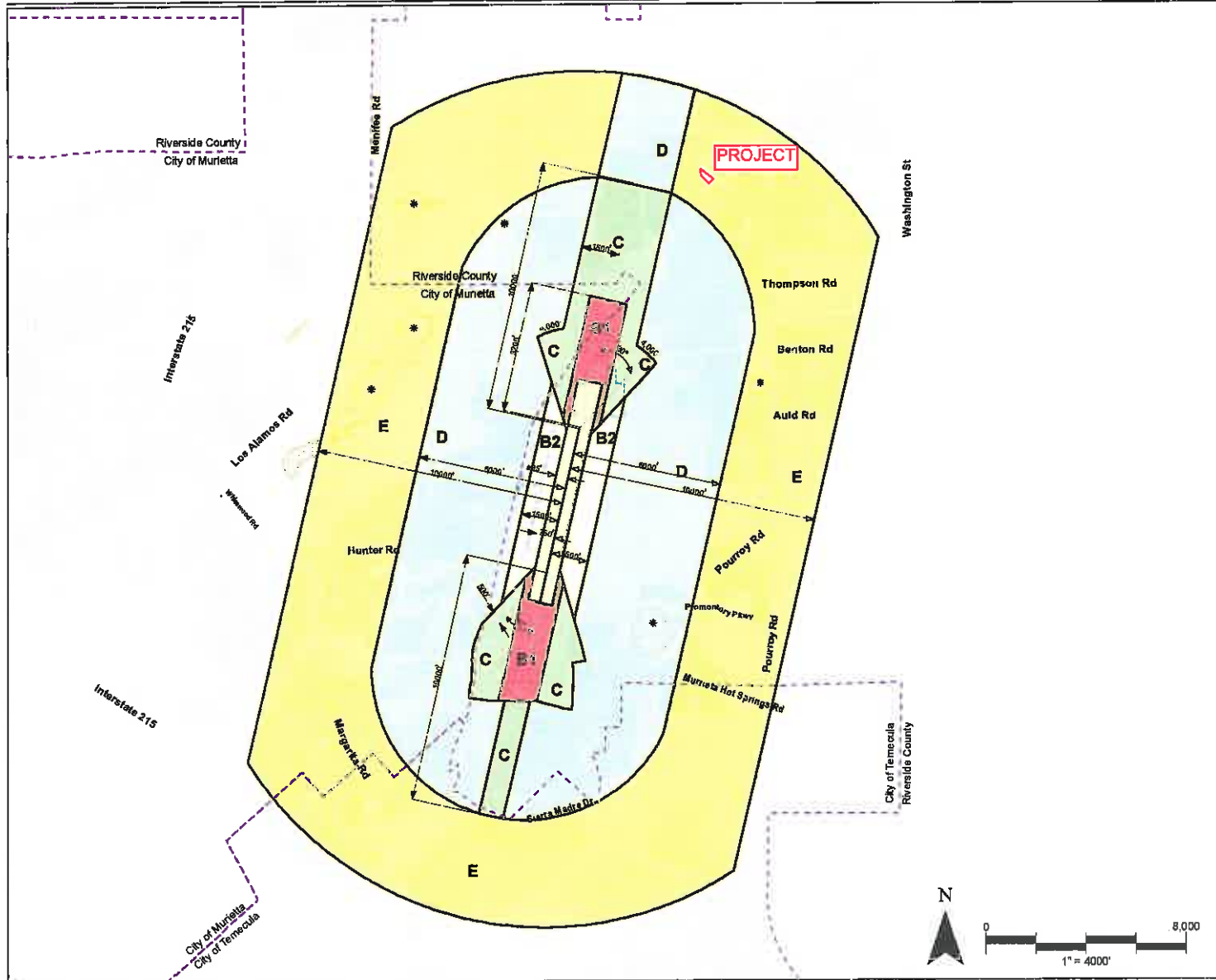
The previous review of the proposed buildings under ZAP1096FV20 found that the building pad elevation is 1,396 feet AMSL, with a maximum building height of 29 feet, resulting in a top point elevation of 1,425 feet. Therefore, review of the proposed buildings by the FAA Obstruction Evaluation Service (FAA OES) was not required.

Open Area: Compatibility Zone E does not require land to be set aside as open areas, and the site is less than 10 acres in area.

Change of Zone: The applicant proposes to change the site's 2.94 acres zoning from Industrial Park (I-P) to Manufacturing Service Commercial (M-SC). The proposed change of zone would be as, or more, consistent with the Compatibility Plan as long as the underlying development's is consistent with the compatibility criteria.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



Legend

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E

Boundary Lines

- Airport Property Line
- - - City Limits
- * Height Review Overlay Zone

Note

Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

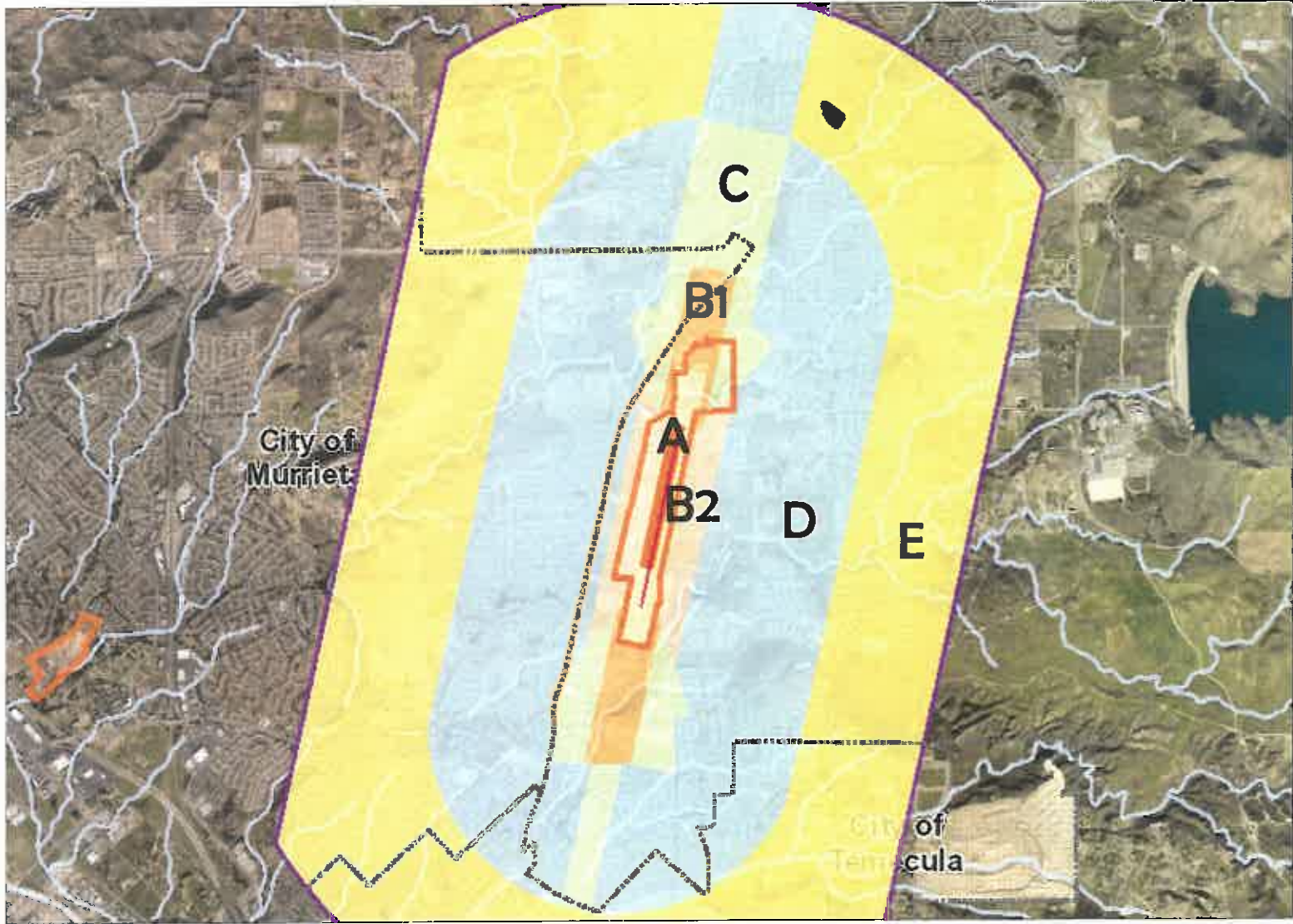
See Chapter 2, Table 2A from compatibility criteria associated with this map.

Riverside County
 Airport Land Use Commission
 Riverside County
 Airport Land Use Compatibility Plan
 Policy Document
 (April 2010)

Map FV-1

Compatibility Map
 French Valley Airport

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY_ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6

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Notes

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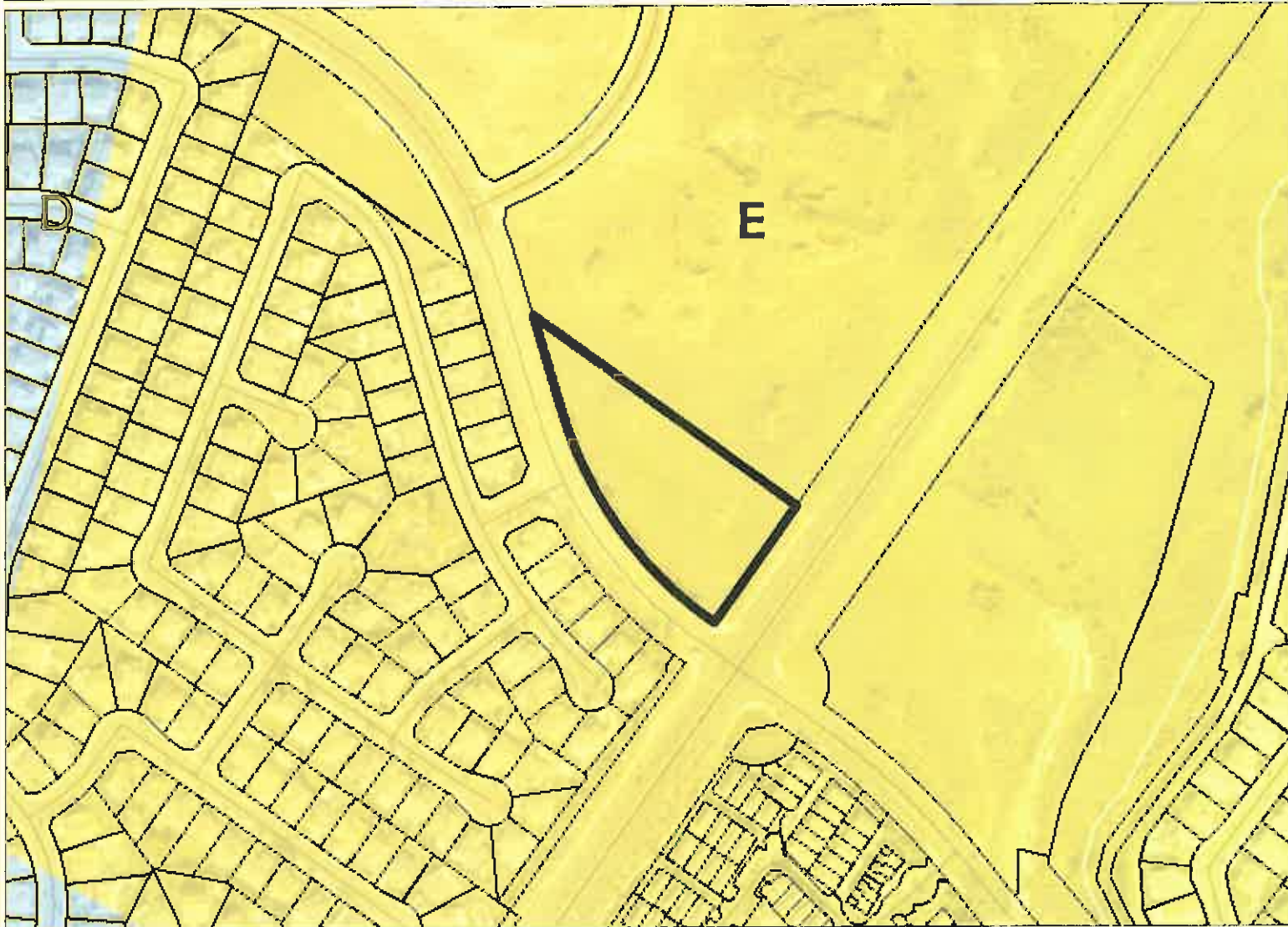
Map My County Map

Los Angeles

San Diego

Tijuana

Mexicali



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5

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Notes



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Map My County Map



Legend

- Parcels
- County Centerlines
- Blueline Streams
- ▤ City Areas
- World Street Map



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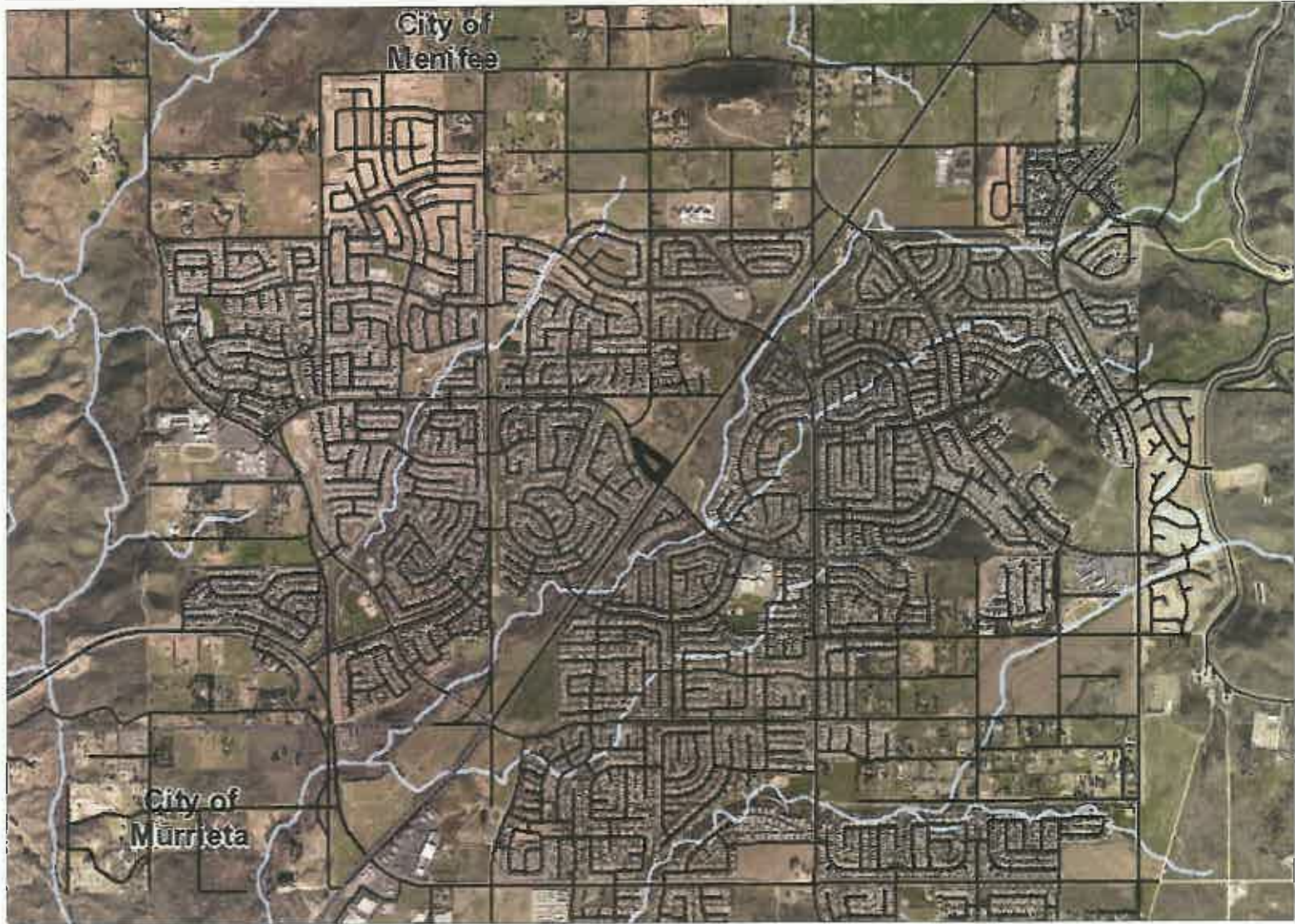
0 376 752 Feet

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Notes

Map My County Map



Legend

- County Centerlines
- Blue Streamlines
- City Areas
- World Street Map

Notes



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Map My County Map



Legend

- Parcels
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map

Notes



0 752 1,505 Feet

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Map My County Map



Legend

- Parcels
- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map

0 188 376 Feet

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Notes

Change of Zone Exhibit

DESERT ZONE
INDUSTRIAL
PART 1.01
WEST TOWN TOWN & COUNTRY
MANUFACTURING
GENERAL COMMERCIAL
UG-20.1



NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
www.rcaluc.org

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The Riverside County Planning Department should be contacted on non-ALUC issues. For more information please contact Riverside County Planner Ms. Deborah Bradford at (951) 955-6646.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to prull@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please telephone Barbara Santos at (951) 955-5132.

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Riverside California

DATE OF HEARING: May 13, 2021

TIME OF HEARING: 9:30 A.M.

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CASE DESCRIPTION:

ZAP1111FV21 – AZIZ, LLC (Representative: Michael Ramirez) – County of Riverside Case No. CZ2100009 (Change of Zone). A proposal to change the zoning of 2.94 gross acres located on the northwest corner of Jean Nicholas Road and Winchester Road from Industrial Park (I-P) to Manufacturing Service Commercial (M-SC) (A previous proposal to construct a retail and fueling facility on this site was found consistent by the ALUC) (Airport Compatibility Zone E of the French Valley Airport Influence Area).



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP 1111 FVZ1 DATE SUBMITTED: 3/31/21

APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

F.V.
ZONE
E.

Applicant	AZIZ LLC	Phone Number	(858)729-3491
Mailing Address	495 E Rinon Ave Ste 175 Corona, CA 92879	Email	mamoil.amiraziz@gmail.com
Representative	Michael Ramirez	Phone Number	760-810-8548
Mailing Address	4300 Edison Ave, Chino, CA 91710	Email	mramirez@beyondfoodmart.com
Property Owner	AZIZ LLC	Phone Number	(858)729-3491
Mailing Address	495 E Rinon Ave Ste 175 Corona, CA 92879	Email	mamoil.amiraziz@gmail.com

LOCAL JURISDICTION AGENCY

Local Agency Name	TRANSPORTATION AND LAND MANAGEMENT AGENCY	Phone Number	951-955-6646
Staff Contact	DEBORAH BRADFORD	Email	DBRADFOR@RIVCO.ORG
Mailing Address		Case Type	CUP / ZONE CHANGE
Local Agency Project No	PPT210004 (FKA CUP 200002) CHANGE OF ZONE (2 210000A)	<input type="checkbox"/> General Plan / Specific Plan Amendment <input type="checkbox"/> Zoning Ordinance Amendment <input type="checkbox"/> Subdivision Parcel Map / Tentative Tract <input checked="" type="checkbox"/> Use Permit <input type="checkbox"/> Site Plan Review/Plot Plan <input type="checkbox"/> Other	

PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address	NW corner of Jean Nicholas Rd & Winchester Rd, Winchester, CA 92596		
Assessor's Parcel No.	480-462-004	Gross Parcel Size	2.94 Acres
Subdivision Name		Nearest Airport and distance from Airport	French Valley Airport / 2 miles
Lot Number			

PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe)	Vacant Land - SEE ATTACHED PROJECT PLANS

Proposed Land Use (describe)	New construction of a service station with canopy, underground storage tanks, a gas station, food mart store with QSR, an express car wash and STARBUCKS with drive thru		
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units) _____		
For Other Land Uses (See Appendix C)	Hours of Operation	24 Hours	
	Number of People on Site	Maximum Number	_____
	Method of Calculation	_____	
Height Data	Site Elevation (above mean sea level)	1398	ft.
	Height of buildings or structures (from the ground)	27 feet high	ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?		<input type="checkbox"/> Yes
	If yes, describe _____ _____		<input checked="" type="checkbox"/> No

- A. **NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
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 1. Detailed project description
 1. Local jurisdiction project transmittal
 3. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
 3. Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. **(Only required if the project is scheduled for a public hearing Commission meeting)**

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.2

HEARING DATE: May 13, 2021

CASE NUMBER: ZAP1106FV21 – Pierer Immoreal North America, LLC
(Representative: CASC Engineering and Consulting)

APPROVING JURISDICTION: Riverside County

JURISDICTION CASE NO: PPT180022S01 (Plot Plan Substantial Conformance)

LAND USE PLAN: 2007 French Valley Airport Land Use Compatibility Plan, as amended in 2011

Airport Influence Area: French Valley Airport

Land Use Policy: Zones B2 and D

Noise Levels: 55-65 CNEL

MAJOR ISSUES: The project’s single acre intensities are inconsistent with the Compatibility Zones B2 and D single acre intensity criteria when the building occupancy is calculated using the State’s Building Code.

The applicant has provided an occupancy survey study of another similar KTM campus in the area. The data in the study reveals the most realistic conservative occupancy of 36 people for the headquarters building, and 32 people for the motorsport building, are significantly lower than the occupancy calculated for the proposed two buildings when using the Building Code method (992 people for the headquarters building, and 516 people for the motorsport building).

If we applied the survey data to the proposed project, the Zone B2 motorsport building would result in an average intensity of 3 people per acre and 32 people in a single acre, and the Zone D headquarters building would result in an average intensity of 4 people per acre and 36 people in a single acre, both of which are consistent with the Zone B2 and D average and single acre criteria.

RECOMMENDATION: Staff recommends that the Commission find the Plot Plan Substantial Conformance CONDITIONALLY CONSISTENT, based on the provided occupancy survey study, subject to the conditions included herein, and such additional conditions as may be required by the Federal Aviation Administration Obstruction Evaluation Service.

PROJECT DESCRIPTION: A proposal to construct a KTM motorsport research and development facility including a two-story 58,351 square foot headquarters office building, a 68,741 square foot motorsport research building, and a 23,697 square foot warehouse building (with an outdoor 20,696 square foot semi-truck parking area and an outdoor 8,602 square foot maintenance area) on 19.57 acres.

On June 13, 2019 (ZAP1083FV18) a proposal to construct a two-story 47,675 square foot headquarters office building, a 60,860 square foot motorsport research building, and a 17,917 square foot warehouse building (with an outdoor 20,696 square foot semi-truck parking area and an outdoor 8,602 square foot maintenance area) on 19.57 acres of this site was reviewed by ALUC and determined conditionally consistent. (A subsequent tentative parcel map under ZAP1100FV20 to divide 31.86 acres into two parcels was also found consistent by the Commission on June 4, 2020)

PROJECT LOCATION: The site is located easterly of Winchester Road (State Highway Route 79), westerly of Sky Canyon Road, northerly of Borel Road, and southerly of Sparkman Way, within the unincorporated community of French Valley, approximately 1,231 feet westerly of the southerly end of Runway 18-36 at French Valley Airport.

BACKGROUND:

Non-Residential Average Intensity: Pursuant to the French Valley Airport Land Use Compatibility Plan, the project site is located within Compatibility Zones B2 (10.12 acres) and D (9.46 acres). Zone B2 restricts average intensity to 100 people per acre, and Zone D restricts average intensity to 150 people per acre through French Valley Airport Compatibility Plan Policy 2.4.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, the following rates were used to calculate the occupancy for the proposed building in Compatibility Zones B2 and D:

- office – 1 person per 200 square feet,
- manufacturing – 1 person per 200 square feet,
- conference room/reception area/break area – 1 person per 15 square feet,
- vocation training room – 1 person per 50 square feet,
- warehouse – 1 person per 500 square feet, and
- storage – 1 person per 300 square feet.

The proposed project includes a two-story 58,351 square foot headquarters office building, a 68,741 square foot motorsport research building, a 23,697 square foot warehouse building, a 20,696 square foot outdoor semi-truck parking area, and an outdoor 8,602 square foot maintenance area (which includes 13,153 square feet of office area, 36,759 square feet of manufacturing area, 16,572 square feet of warehouse area, 17,437 square feet of storage area, 15,568 square feet of conference room/reception/break area, 5,342 square feet of vocation training room, and 14 outdoor entrance bays for truck parking and maintenance), accommodating a total occupancy of 1,508 people and

resulting in an average intensity of 77 people per acre which is consistent with the Compatibility Zones B2 average criterion of 100 and Compatibility Zone D criterion of 150.

A breakdown of use by Compatibility Zone indicates that Zone B2 would contain the motorsport building, warehouse, and outdoor entrance bays for truck parking and maintenance, which includes 4,303 square feet of office area, 33,269 square feet of manufacturing area, 3,425 square feet of lobby/breakroom area, 16,572 square feet of warehouse area, 13,643 square feet of storage area, and 14 outdoor entrance bays for truck parking and maintenance, accommodating 516 people, resulting in an average intensity of 51 people per acre, which is consistent with the Compatibility Zone B2 average criterion of 100.

The Zone D portion of the site would contain the two-story headquarters building, which includes 8,850 square feet of office area, 3,490 square feet of manufacturing area, 12,143 square feet of conference room/reception/breakroom area, 5,342 square feet of vocation training room area, and 3,794 square feet of storage area, accommodating 992 people, resulting in an average acre intensity of 105 people, which is consistent with the Compatibility Zone D criterion of 150.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per standard vehicle). Based on the number of standard parking spaces provided of 272 spaces, the total occupancy would be estimated at 408 people, resulting in an average intensity of 21 people per acre for the entire site, which is consistent with both the Zone B2 average criterion of 100 and the Zone D criterion of 150.

Non-Residential Single-Acre Land Use Intensity: Pursuant to the French Valley Airport Land Use Compatibility Plan, the project site is located within Compatibility Zones B2 and D. Zone B2 restricts single acre intensity to a maximum of 200 people, and Zone D restricts single acre intensity to 450 people in the most intensely utilized acre, through French Valley Airport Compatibility Plan Policy 2.4.

Based on the site plan provided and the occupancies as previously noted, the single-acre intensities of concern would occur within a portion of the motorsport research and development building in Compatibility Zone B2, and entirely consist of the two-story headquarters building in Compatibility Zone D.

The single acre intensity for the motorsport building located entirely within Compatibility Zone B2 would include 4,796 square feet of office area, 17,307 square feet of manufacturing area, 5,326 square feet of storage area, and 3,425 square feet of break room/lobby area, accommodating a single acre occupancy of 357 people, which is inconsistent with the Compatibility Zone B2 criterion of 200.

The single acre intensity for the two-story headquarters building located entirely within Compatibility Zone D would include 8,850 square feet of office area, 3,490 square feet of manufacturing area, 3,794 square feet of storage area, 12,143 square feet of conference room/lobby area, and 5,342 square feet of vocational training area, accommodating a single-acre occupancy of

992 people, which is inconsistent with the Compatibility Zone D criterion of 450.

KTM North America Campus – Occupancy Survey of Similar Facility: In light of the project's inconsistent single acre intensities (per the Building Code Method) in Zones B2 and D, the applicant prepared a survey study examining the occupancies of a similar campus.

The study highlights the fact that the building code method used for determining site intensity is a conservative approach in calculating actual project intensity. As such, the study examines one similar existing campus facility, spread over 4 separate buildings located at 38415 Innovation Court, 38429 Innovation Court, 38340 Innovation Court, and 30100 Technology Drive, Murrieta, for a more realistic occupancy generation calculation.

The study examines swipe card data collected between October 1, 2019 and December 31, 2019, identifying the following:

- highest daily total of 461 people for the entire (4 building) campus facility,
- highest daily total of 227 people for just the 2-building headquarters, and
- highest daily total of 234 people for just the 2-building motorsports.

Additionally, the study also provides highest hourly totals (occurring on December 17, 2019, at 11:00 a.m.), identifying the following:

- highest hourly total of 68 people for the entire (4 building) campus facility,
- highest hourly total of 36 people for just the 2-building headquarters,
- highest hourly total of 32 people for just the 2-building motorsports.

All of these results are significantly lower compared to the occupancy calculated using the Building Code method of 1,508 people for the proposed KTM facility (516 people for the motorsport building, 992 people for the headquarter building).

If we applied the highest hourly total from the study of 36 people for the headquarter buildings and 32 people for the motorsport buildings, the proposed project would accommodate a total occupancy of 68 people. The Zone B2 motorsport building would result in an average intensity of 3 people per acre and 32 people in a single acre which are consistent with the Zone B2 average criteria of 100 people per acre and single acre criteria maximum of 200 people. The Zone D headquarters building would result in an average intensity of 4 people per acre and 36 people in a single acre which are consistent with the Zone D average criteria of 150 people per acre and single acre criteria maximum of 450 people.

It is important to note that the existing 81,723 square foot KTM campus facility is significantly smaller than the proposed 150,780 square foot KTM project. However, if we were to double the building size and occupancy of the existing facility, the resulting 163,446 square foot facility (which is more comparable to the proposed KTM project size) would result in a total occupancy of 136 people, 72 people in the headquarters building, and 64 people in the motorsports building. The resulting intensities would still be consistent with the Zones B2 and D average and single acre intensity criteria.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zones B2 or D (children's schools, day care centers, libraries, hospitals, nursing homes, places of worship, highly noise-sensitive outdoor non-residential uses, hazardous materials and hazards to flight), other than the proposed prohibited nonresidential intensity.

Noise: The French Valley Compatibility Plan depicts the site as being located within the 55-65 CNEL contour range from aircraft noise. Office and industrial uses are identified as normally and marginally acceptable within the 55-65 CNEL contour range. The indoor sensitive uses like office areas would be impacted by aircraft generated noise, and, therefore, staff is recommending a condition to incorporate noise attenuation measures into the design of these areas to such extent as may be required to ensure that interior noise levels from aircraft operations are at or below 45 CNEL.

Part 77: The elevation of Runway 18-36 at its southerly terminus is 1,340 feet above mean sea level (AMSL). At a distance of approximately 1,231 feet from the runway to the project boundary, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,352.3 feet AMSL.

The original project's finished floor elevation was 1,327.5 feet AMSL. With a maximum building height of 29 feet, the top point elevation was 1,356.5 feet. Therefore, review of the buildings by the FAA Obstruction Evaluation Service (FAAOES) was required. The applicant submitted to the FAAOES and Aeronautical Study Numbers 2018-AWP-15606-OE (motorsport building), 2018-AWP-15608-OE (headquarters building), and 2019-AWP-5221-OE (warehouse) were assigned to the project. The FAA OES issued "Determination of No Hazard to Air Navigation" letters dated November 1 and 2, 2018 for the motorsport building and headquarter building, and July 11, 2019 for the warehouse building.

As part of the revised project, the applicant is proposing to increase the height of the headquarters building from 29 feet to 37 feet (There are no changes to the motorsport building height, therefore the original FAA no hazard determination letter 2018-AWP-15606 is still valid). The applicant submitted Form 7460-1 to the FAA OES for the headquarters building, and the FAA OES assigned Aeronautical Study Number 2020-AWP-5214-OE to this proposal. The aeronautical study revealed that the proposed structure would not exceed obstruction standards and would not be a hazard to air navigation, provided conditions are met. Therefore, FAA OES issued a "Determination of No Hazard to Air Navigation" letter on January 8, 2021.

The applicant is also proposing an increase in height for the warehouse building from 26 feet to 30 feet. The applicant has submitted a new FAA application for the increase warehouse building height. The FAA OES assigned Aeronautical Study Number 2021-AWP-5927-OE to this proposal. At the time the staff report was written, no determination has been made, but the study is in a "Work in Progress" status.

Open Area: The project site is split between Compatibility Zones B2 and D, with approximately 10.12 acres in Zone B2 and 9.46 acres in Zone D. Compatibility Zone D requires 10% (9.46 acres) of the land area within major projects (10 acres or larger) be set aside as open area that could potentially serve as emergency landing areas (Compatibility Zone B2 does not require any amount of open area due to its proximity to the actual runway.) The original project identified 2.46 acres of ALUC eligible open areas in Zone D consisting of driveway aisles and parking lot areas within the proposed development. The project is conditioned to maintain these areas consistent with ALUC open area requirements of 300 feet by 75 feet minimum shape and prohibit obstructions greater than 4 feet in height that are at least 4 inches in diameter. The proposed project revisions will not impact the original project's provision of open space area.

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33B).

The nearest portion of the project is located 1,231 feet from the runway, and therefore would be subject to the above requirement. The project would include a bioinfiltration basin, located entirely within Zone D. Such basins are to be avoided in Zone D due to the potential that such areas could provide food, water, and shelter for hazardous wildlife. Pursuant to the study "Wildlife Hazard Management at Riverside County Airports: Background and Policy", October 2018, by Mead & Hunt, which is the basis of the brochure titled "Airports, Wildlife and Stormwater Management", such basins are to be avoided in Zones D and E, unless they provide for a 48-hour drawdown and propose landscaping that is not attractive to hazardous wildlife. The project has been conditioned to be consistent with these standards in order to reduce bird attractant: 1) new basins are to be designed so as to provide for a maximum 48-hour detention period following the conclusion of a storm event, and to remain totally dry between rainfalls, and 2) any landscaping proposed in the detention basin shall be in accordance

CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight

final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.

- (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, outdoor production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, day care centers, libraries, hospitals, nursing homes, places of worship, buildings with more than two aboveground habitable floors, critical community infrastructure facilities, and aboveground bulk storage of 6,000 gallons or more of flammable or hazardous materials.
 - (f) Highly noise-sensitive outdoor nonresidential uses.
 - (g) Any use which results in a hazard to flight, including physical (e.g., tall objects), visual, and electronic forms of interference with the safety of aircraft operations.
3. Prior to issuance of building permits, the property owner shall convey an avigation easement to French Valley Airport. Copies of the recorded avigation easement shall be forwarded to the Airport Land Use Commission and to the County of Riverside.
 4. The attached notice shall be provided to all prospective purchasers of the property and future tenants of the buildings thereon.
 5. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the stormwater basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the

“AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT” brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: “There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes”. The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

6. This project has been evaluated as consisting of 13,153 square feet of office area, 36,759 square feet of manufacturing area, 16,572 square feet of warehouse area, 17,437 square feet of storage area, 15,568 square feet of conference room/reception/break area, 5,342 square feet of vocation training room, and 14 outdoor entrance bays for truck parking and maintenance. Any increase in building area, height, change in use to any higher intensity use, change in building location or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.
7. Noise attenuation measures shall be incorporated into the design of the buildings, to the extent such measures are necessary to ensure that interior noise levels from aircraft operations are at or below 45 CNEL.
8. The ALUC open areas as shown on the site plan shall be devoid of obstacles/obstructions greater than 4 feet in height that are at least 4 inches in diameter, which includes parking light poles, walls, trash enclosures, and tall landscaping.
9. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission and Riverside County as owner and operator of French Valley Airport. In the event of any reasonable complaint about glare related to aircraft operations, the applicant shall agree to such specific mitigation measures as determined or requested by Riverside County.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

THERE IS AN AIRPORT NEARBY.

**THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS**

**PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES**



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2020-AWP-12888-OE
Prior Study No.
2019-AWP-5214-OE

Issued Date: 01/08/2021

Cheryl Webb
Pierer Immoreal North America
38429 Innovation Court
Murrieta, CA 92563

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building KTM Headquarters - Revised 11.18.2020
Location: Murrieta, CA
Latitude: 33-34-17.05N NAD 83
Longitude: 117-07-59.66W
Heights: 1320 feet site elevation (SE)
40 feet above ground level (AGL)
1360 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 07/08/2022 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (206) 231-2877, or Nicholas.Sanders@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-AWP-12888-OE.

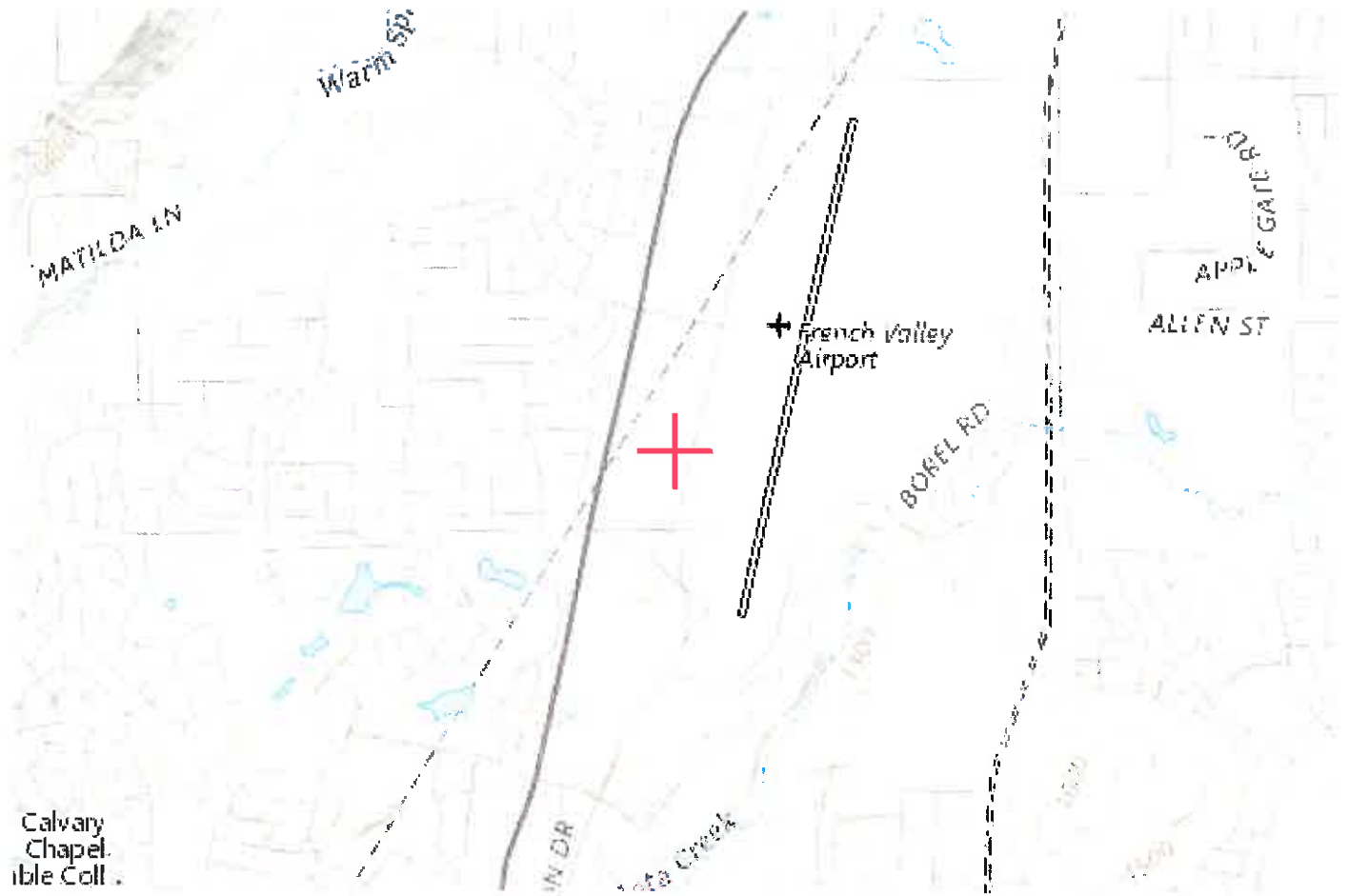
Signature Control No: 456970422-462969602

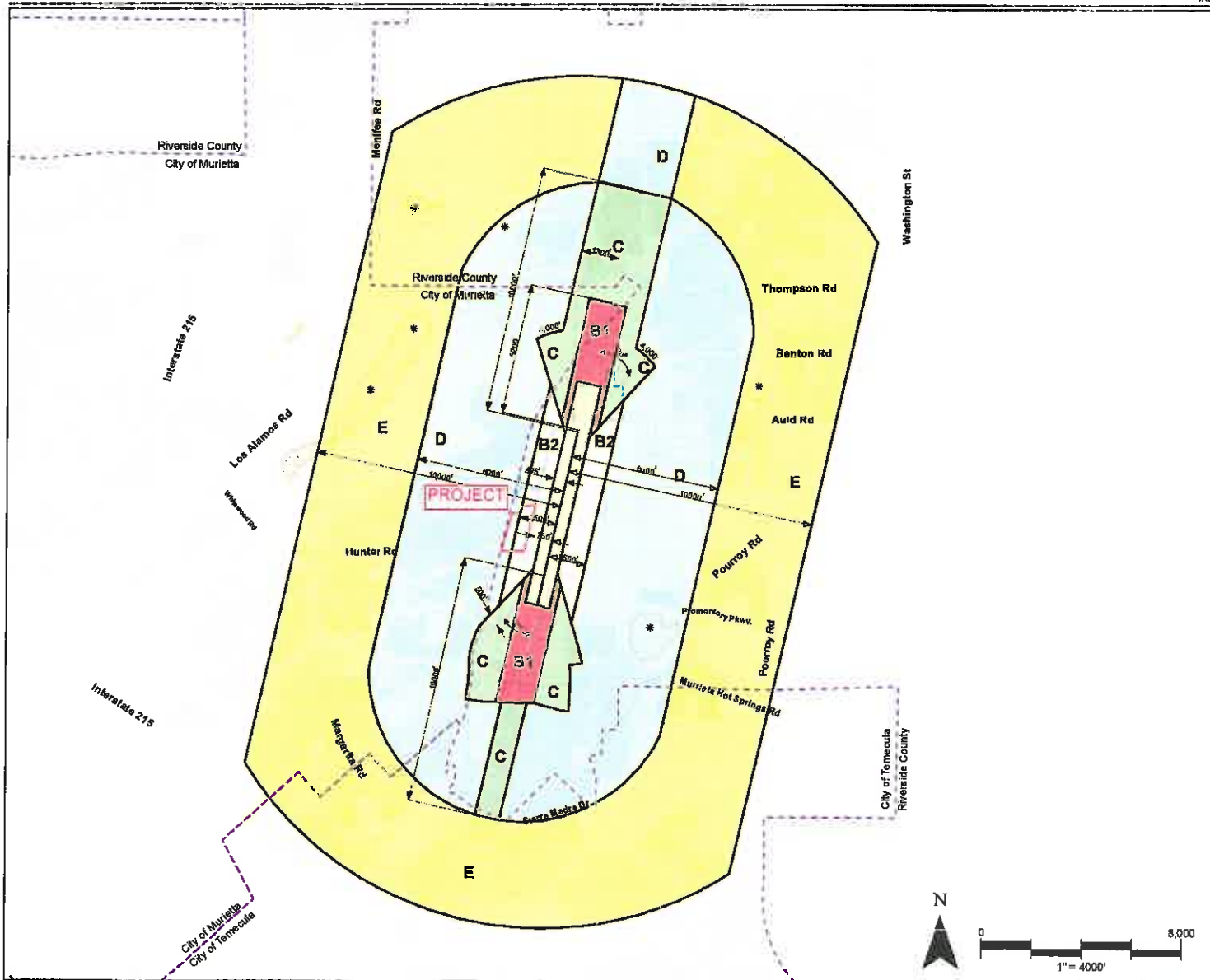
(DNE)

Nicholas Sanders
Technician

Attachment(s)
Map(s)

TOPO Map for ASN 2020-AWP-12888-OE





Legend

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E

Boundary Lines

- Airport Property Line
- - - City Limits
- * Height Review Overlay Zone

Note

Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

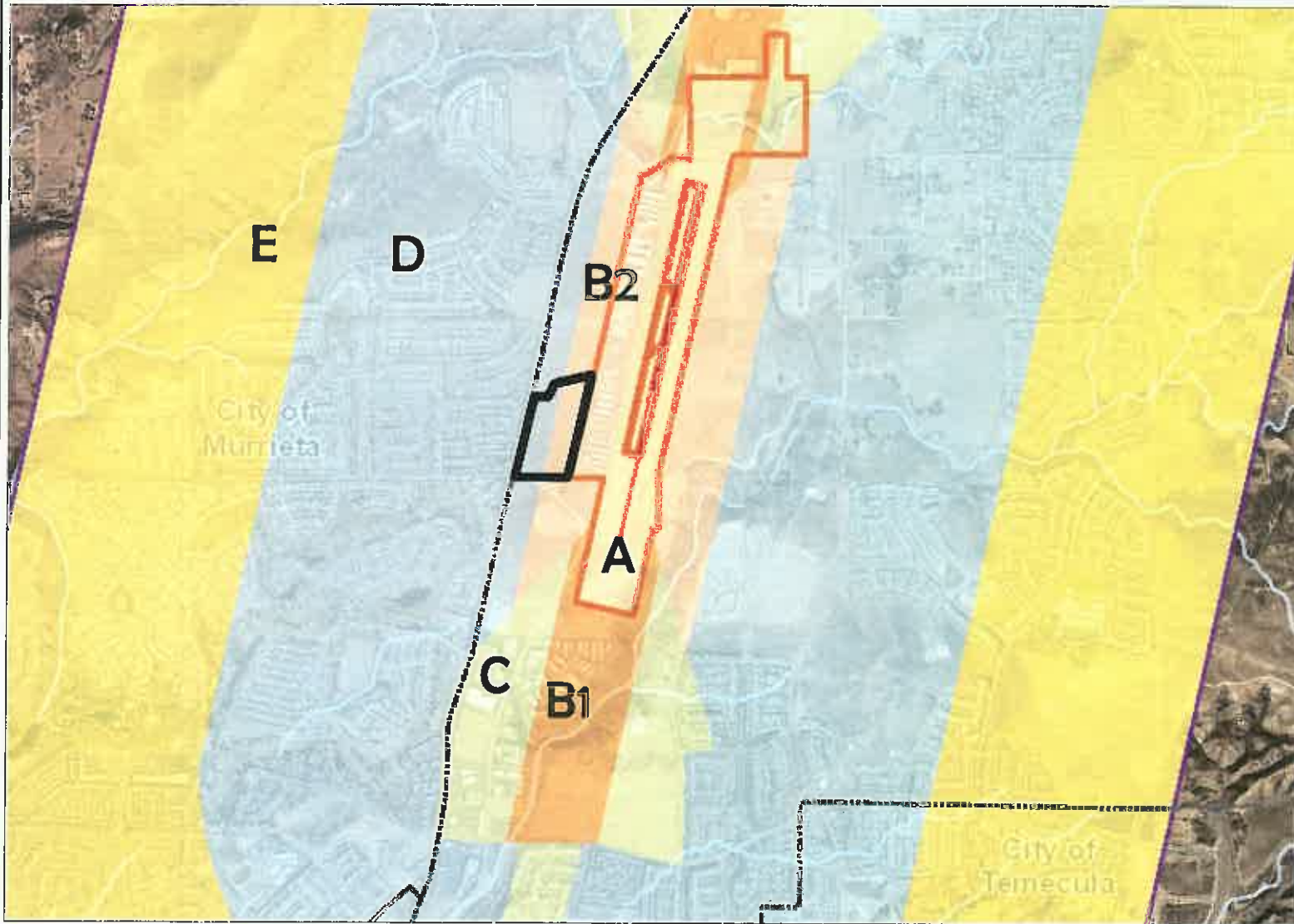
See Chapter 2, Table 2A from compatibility criteria associated with this map.

Riverside County
 Airport Land Use Commission
 Riverside County
 Airport Land Use Compatibility Plan
 Policy Document
 (April 2010)

Map FV-1




Compatibility Map
 French Valley Airport

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6

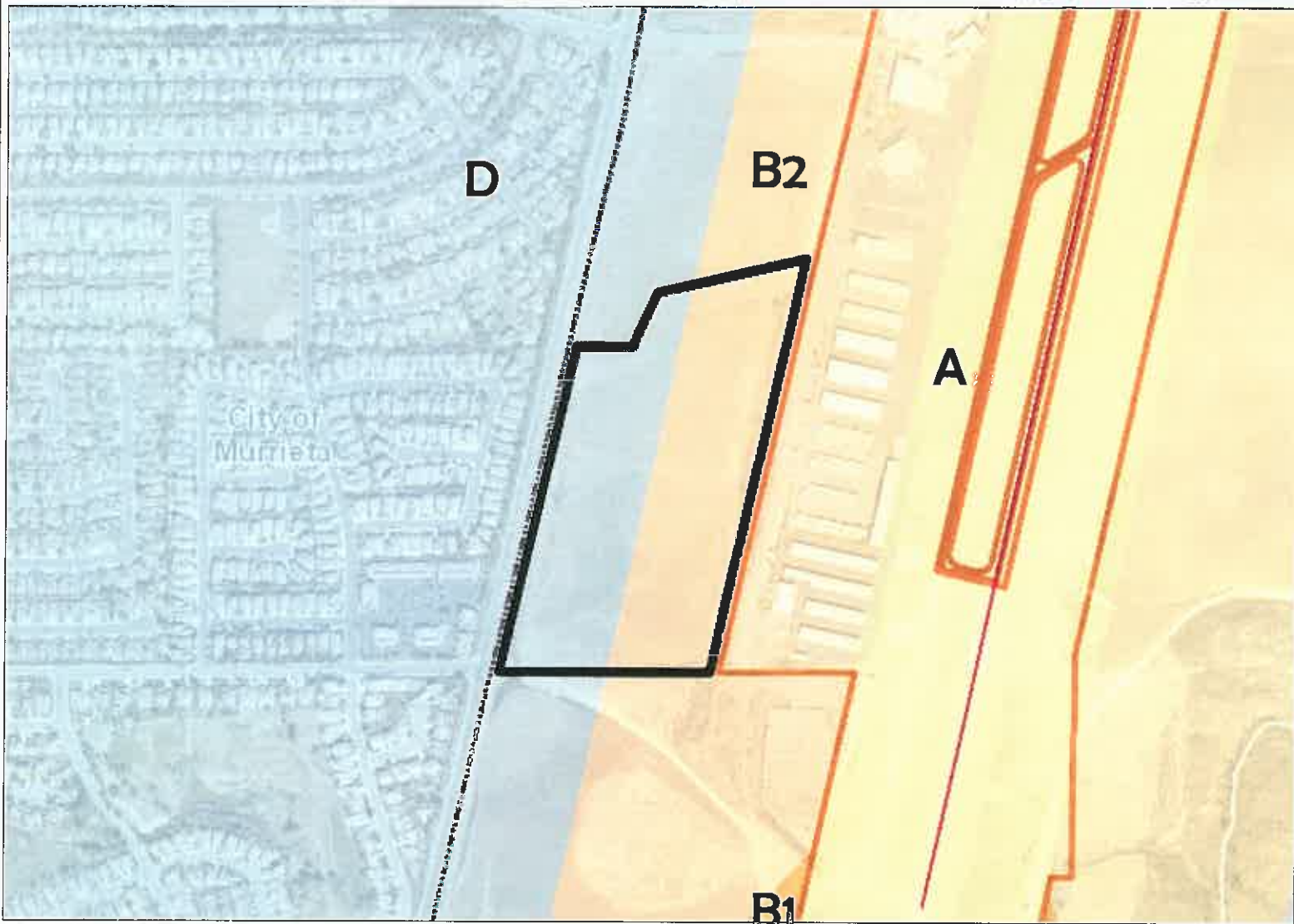




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Notes

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- ▨ OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-FXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-FXC6



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Notes

Map My County Map



Legend

- Blue line Streams
- City Areas
- World Street Map

Notes



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Map My County Map



Legend

-  Blue Line Streams
-  City Areas
-  World Street Map



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Notes

Map My County Map



Legend

- Blueline Streams
- City Areas
- World Street Map



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Notes

0 1 3,079 Feet
539

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Map My County Map



Legend

- Blue line symbol: Blueline Streams
- Grid symbol: City Areas
- World Street Map symbol: World Street Map

Notes



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To: Paul Ruli, ALUC Director
Riverside County Airport Land Use Commission (ALUC)

From: Frank Coyle, Director of Planning

Date: April 19, 2021

Re: **ALUC Case ZAP1106FV21, KTM North America Campus - Survey of Similar Uses**

CASC understands that the Riverside Airport Land Use Commission (ALUC) will be evaluating changes to the Pierer Immoreal North America, LLC (PINA, Applicant) Project (Project) which was originally approved by the Riverside Airport Land Use Commission (ALUC) on June 13, 2019 (ZAP1100FV20). We have reviewed the Project changes and based on a detailed Survey of Similar Uses prepared for the Project, the proposed modifications to the Project are consistent with the French Valley Land Use Compatibility Plan.

Project Background

On June 13, 2019, ALUC determined that the original KTM North America campus,¹ consisting of a main Headquarters (47,675 sq. ft.), Motorsport (60,860 sq. ft.), and Storage (17,917 sq. ft.) buildings for a total building square footage of 126,452 sq. ft., was consistent with the 2007 French Valley Airport Land Use Compatibility Plan. At this time, the Applicant desires to increase the building square footage of the three buildings and to add certain outdoor facilities. The Applicant is processing an application for Substantial Conformance to the approved Plot Plan for the original Project with the Riverside County Planning Department. The Planning Department is currently reviewing the application.

The proposed modifications to the Project will result in the development of an office building complex with a gross square footage of 150,789 square feet on 31.86 acres. The three buildings proposed include a main Headquarters building of 58,351 square feet, a Motorsport building of 68,741 square feet, and a Storage building of 23,697 square feet. Additionally, 20,696 square feet is proposed for open semi-truck parking, 8,602 square feet is proposed for a maintenance intake area, and 1,300 square feet is proposed for open tire storage. The size of the proposed modified project is larger than the existing facility to only allow more space for storage, equipment, work areas, and single person offices. The increase in gross square feet is proposed to increase the efficiency and functionality of the current workforce. The buildings will not experience an increase in employees because of the increase in size.

The Project is located at 37862 Winchester Road, Murrieta, California 92563. The property is identified as Accessor's Parcel Number (APN) 963-030-013 (formerly APN 963-030-002). The property has an underlying land use designation of Commercial Office/Commercial Retail and a zoning classification of Specific Plan (SP) Borel Airpark Center (SP No. 265).

Site Intensity

Determining the site intensity for the Airport Land Use Commission (ALUC) is typically calculated using the building code method pursuant to *Appendix C, Table C-1*, of the Riverside County ALUC Plan. However, as made clear in *Appendix C*, utilization of the building code method may in some cases overestimate potential intensity levels compared to actual occupancy levels. The Project site is situated within

¹ KTM is an affiliate of PINA

Compatibility Zone B2 and Compatibility Zone D of the French Valley Airport Land Use Compatibility Plan. Compatibility Zone B2 identifies an Average Acre requirement of 100 people per acre and a Single Acre requirement of 250 people per acre. Compatibility Zone D identifies an Average Acre requirement of 150 people per acre and a Single Acre requirement of 450 people per acre. Based on the building code method, the Motorsports building exceeds the single-acre intensity.

Alternative methods for determining site intensity include calculations based on parking ordinance and Survey of Similar Uses. KTM currently operates a portion of their business out of four (4) existing buildings with a gross square footage of approximately 81,723 square feet located on Innovation Court and Technology Drive, within one mile of the proposed Project. The current business operation has 138 employees. A satellite image of the current KTM Facilities is attached here as Exhibit "A". The proposed KTM campus would include up to 175 employees and 25 dealer technicians. A chart summarizing the existing facility is set forth below.

Table A, Existing Operation on Innovation Court and Technology Drive

Building No	Address	Current Use	Gross Square Footage Used	Equivalent Use
1	38415 Innovation Court (Suite A-N), Murrieta, CA, 92563	Tech Center, Marketing, RMC Team, Media, Cust. Serv., Dealer Tech Serv., Recon, HQV Off Road, Storage	36,120 sq. ft.	Headquarters Building
2	30100 Technology Drive, Murrieta, CA, 92563	Group HQ Executive Conference Rooms, Sales, HQV SX/MX Offices, Warehouse S&R, Parts Storage, Acct/Finance, Legal, Inside Sales	15,100 sq. ft.	Headquarters Building
Gross Square Footage			51,220 sq. ft.	
3	38429 Innovation Court, Murrieta, CA, 92563	KTM Motorsports, Dealer Development, E-bike Team offices, Parts Storage, Homologation Offices, Human Resources, Executive Offices	21,504 sq. ft.	Motorsports Building
4	38340 Innovation Court (Suite 701-703), Murrieta, CA, 92563	R&D, WP, Parts Equip, Storage	8,999 sq. ft.	Motorsports Building
Gross Square Footage			30,503 sq. ft.	

As shown in **Table A**, Buildings 1 through 4 are sorted into two categories, Headquarters and Motorsports, which represent the future buildings uses for the modified project. Building numbers 1 and 2 serve the same use as the proposed Headquarters building, and building numbers 3 and 4 serve the same purpose

as the proposed Motorsports building. The 138 employees who currently work out of Buildings 1 through 4 would work out of the two buildings proposed for the modified Project.

Table B, Comparison of Development, shows a side by side comparison of the distribution of square feet currently used for Headquarters and Motorsports use, the gross size of the original development plan previously approved by ALUC, and the gross size of the proposed modified project for consideration by ALUC now.

Table B, Comparison of Development

Building	Existing Facility	Original Project ZAP1106FV20	Modified Project ZAP1106FV21
Headquarters	51,220 sq. ft.	47,675 sq. ft.	58,351 sq. ft.
Motorsports	30,503 sq. ft.	60,860 sq. ft.	68,741 sq. ft.
Storage	-	17,917 sq. ft.	23,697 sq. ft.
Gross Square Footage	81,723 sq. ft.	126,452 sq. ft.	150,789 sq. ft.

Survey of Similar Uses

To prepare a Survey of Similar Uses for the proposed Project, the Applicant has analyzed three months of data from “swipe card readers” on the four Innovation Court buildings KTM currently operates with its existing workforce of 138 employees. As shown above in **Table A**, the four existing buildings serve the Applicant for two uses, Headquarters and Motorsports. To provide an accurate projection of the number of employees to the proposed Headquarters and Motorsports buildings on APN 963-030-013, **Table C** categorizes the data from four buildings into the respective two categories.

This study analyzes foot traffic during a three month time period, October 1, 2019 through December 31, 2019, (“Study Period”) as this is the busiest time of the year for employees to the facility. **Table C** demonstrates that during the busiest quarter of the year, the Headquarters building experiences a peak daily average of 26 employees throughout the day, and the Motorsports building experiences a peak daily average of 27 employees throughout the day. Overall, on any given weekday, there are no more than an average of 53 employees out of the existing 138 employees on the campus in a day. Thus, there are no more than 38% of the employees on the campus during the busiest time of the week.

Table C, Average Number of Employees per Weekday

Day of Week	Average Daily Number of Employees on Existing Campus		
	Headquarters	Motorsports	Total
Monday	24	27	51
Tuesday	26	27	53
Wednesday	26	25	51
Thursday	24	23	47
Friday	21	19	40
Saturday	1	1	2
Sunday	0	1	1

Swipe data is registered with a date and time stamp, along with an employee “Badge” number to identify who is entering each building and at what time of the day. The “worst case scenario” is derived from the peak number of employees to visit the buildings in one hour on any day in the Study Period. The day with the “worst case scenario” is December 17, 2019, where 68 employees were present during the 11:00 AM to 12:00 PM hour. As shown below, the Headquarters building experienced a peak hourly average of 36 employees on the busiest day, and the Motorsports building experienced a peak hourly average of 32 employees on the busiest day. Overall, for any given hour, there were no more than 68 employees on campus during a peak period. Thus, there was no more than 50% of employees on campus during a peak hour.

Table D, Worst Case Scenario - Existing Facility Peak Employees per Hour

Date	Hour Starting	Hourly Headcount		
		Headquarters	Motorsports	Total
12/17/2019	12:00 AM	0	0	0
12/17/2019	1:00 AM	0	0	0
12/17/2019	2:00 AM	0	0	0
12/17/2019	3:00 AM	0	0	0
12/17/2019	4:00 AM	0	0	0
12/17/2019	5:00 AM	0	0	0
12/17/2019	6:00 AM	0	0	0
12/17/2019	7:00 AM	0	0	0
12/17/2019	8:00 AM	1	1	2
12/17/2019	9:00 AM	5	9	14
12/17/2019	10:00 AM	25	29	54
12/17/2019	11:00 AM	36	32	68
12/17/2019	12:00 PM	22	20	42
12/17/2019	1:00 PM	10	18	28
12/17/2019	2:00 PM	12	17	29
12/17/2019	3:00 PM	21	23	44
12/17/2019	4:00 PM	29	27	56
12/17/2019	5:00 PM	17	25	42
12/17/2019	6:00 PM	14	25	39
12/17/2019	7:00 PM	7	15	22
12/17/2019	8:00 PM	8	5	13
12/17/2019	9:00 PM	2	2	4
12/17/2019	10:00 PM	1	0	1
12/17/2019	11:00 PM	0	0	0

Average Acre and Single Acre Intensity Calculations

As noted above, there are no more than 38% of employees on the existing KTM campus during a typical day. The “worst case scenario” shows that no more than 50% of employees were present on the campus in one hour. The Project as proposed will have up to 175 employees. As the existing building uses will be the same for the proposed Project, the same occupancy ratios can be applied to the proposed Project.

Thus, there would be no more than 68 out of 175 employees on the campus during a typical day and no more than 88 out of 175 employees during a peak hour.

Table E shows the size of each building and the number of people assigned to work there. The number of employees that would be on the campus is calculated using the percentage of current employees during the “worst case scenario” peak hour which is 50%.

Table E, Average Acre and Single Acre Intensity Compatibility

Building	Square Footage	Compatibility Zone	Average Acre Limit	Single Acre Limit	Number of Employees	Worst Case Scenario Occupancy Rate	Peak # of Employees Onsite ¹
Headquarters	58,351	D	100	300	100	50%	50
Motorsports	68,741	B2	100	200	75		38
Total					175		88

1. Number of Employees x Worst Case Scenario Occupancy Rate = Peak # of Employees Onsite

As illustrated in Table E above, the intensity of employees in any building or area would not exceed the Average Acre or Single Acre intensities at any time based on the conclusions as noted above. The intensity of employees is much lower than the Average and Single Acre Limits. Based on the “worst case scenario”, the intensities would not be exceeded unless KTM employs over 400 employees, which is not in their business plan to have than many employees. Based on the nature of the business and the development proposed, this growth is not anticipated and the Project as proposed would be unable to support that many employees. The proposed offices and workspaces have been designed to support up to 175 employees.

The Motorsports building proposed in Compatibility Zone B2 has a more conservative Single Acre Limit of 200 people per acre. There is additional certainty that this limit will not be exceeded in any event because the Motorsport building requires a high security clearance for entry which is only available to upper management, technicians, and race teams. This includes up to 75 employees. Access is extremely limited by design and other staff would typically not be in this building at any given time. Thus, the proposed workforce of 175 employees and the proposed building square footage modifications would be compatible with the French Valley Airport Land Use Compatibility Plan and the use of the proposed Project is consistent with the Riverside ALUC Plan.

Conclusion

The Survey of Similar Uses Method reflects that the maximum number of employees on-site will be significantly lower than the Average Acre and Single Acre intensities mandated by Compatibility Zone B2 and Compatibility Zone D (See Table D). As the buildings are not open to the public, the number of personnel at the facility is predictable and consistent. The analysis of the existing facility shows that the highest number of employees to enter a building was approximately 53 people on an average weekday.

The “worst case scenario” is represented by busiest day in the Study Period, Tuesday, December 17, 2019, where the peak number of employees per hour was 68 employees, or 50% of the workforce, from 11:00 AM to 12:00 PM. The “worst case scenario” for the proposed Headquarters building (100 employees) would be 46 employees during the peak hour, and the “worst case scenario” for the proposed Motorsports building (75 employees) would be 41 employees during the peak hour. Under the “worst case scenario”, only 87 employees would be in either the Headquarters or Motorsport building. Due to the nature of the business, under no circumstances would all 87 people be in one building. Even if all 87 employees were to visit one building, the Average Acre Limits and Single Acre Limits would not be exceeded. Based on the conclusions in this analysis, we find the Project consistent with the Riverside ALUC Plan and the Average Acre and Single Acre Limits will not be exceeded.

Exhibit "A"



Employee Visits per Day

October 1st, 2019 - December 31st, 2019

Notes: The summary below represents the "Door Swipe" events by unique Employee Badge # for each day in the Study Period. Each day represents the total number of employees to visit the facility on that day. This data does not represent the total number of employees to be in a building at the same time.

Date	Year	Total # of Employees	Weekday	Weekday #	Busiest Day?
12/31/2019	2019	35	Tuesday	2	FALSE
12/30/2019	2019	76	Monday	1	FALSE
12/29/2019	2019	5	Sunday	7	FALSE
12/28/2019	2019	8	Saturday	6	FALSE
12/27/2019	2019	65	Friday	5	FALSE
12/26/2019	2019	66	Thursday	4	FALSE
12/25/2019	2019	2	Wednesday	3	FALSE
12/24/2019	2019	14	Tuesday	2	FALSE
12/23/2019	2019	85	Monday	1	FALSE
12/22/2019	2019	7	Sunday	7	FALSE
12/21/2019	2019	9	Saturday	6	FALSE
12/20/2019	2019	116	Friday	5	FALSE
12/19/2019	2019	127	Thursday	4	FALSE
12/18/2019	2019	133	Wednesday	3	TRUE
12/17/2019	2019	131	Tuesday	2	FALSE
12/16/2019	2019	119	Monday	1	FALSE
12/15/2019	2019	2	Sunday	7	FALSE
12/14/2019	2019	3	Saturday	6	FALSE
12/13/2019	2019	110	Friday	5	FALSE
12/12/2019	2019	124	Thursday	4	FALSE
12/11/2019	2019	129	Wednesday	3	FALSE
12/10/2019	2019	128	Tuesday	2	FALSE
12/9/2019	2019	122	Monday	1	FALSE
12/8/2019	2019	3	Sunday	7	FALSE
12/7/2019	2019	9	Saturday	6	FALSE
12/6/2019	2019	90	Friday	5	FALSE
12/5/2019	2019	119	Thursday	4	FALSE
12/4/2019	2019	123	Wednesday	3	FALSE
12/3/2019	2019	127	Tuesday	2	FALSE
12/2/2019	2019	118	Monday	1	FALSE
12/1/2019	2019	4	Sunday	7	FALSE
11/30/2019	2019	2	Saturday	6	FALSE
11/29/2019	2019	11	Friday	5	FALSE
11/28/2019	2019	1	Thursday	4	FALSE
11/27/2019	2019	85	Wednesday	3	FALSE
11/26/2019	2019	111	Tuesday	2	FALSE
11/25/2019	2019	103	Monday	1	FALSE
11/24/2019	2019	3	Sunday	7	FALSE
11/23/2019	2019	5	Saturday	6	FALSE
11/22/2019	2019	73	Friday	5	FALSE
11/21/2019	2019	116	Thursday	4	FALSE
11/20/2019	2019	123	Wednesday	3	FALSE
11/19/2019	2019	113	Tuesday	2	FALSE
11/18/2019	2019	98	Monday	1	FALSE
11/17/2019	2019	2	Sunday	7	FALSE

11/16/2019	2019	5	Saturday	6	FALSE
11/15/2019	2019	84	Friday	5	FALSE
11/14/2019	2019	101	Thursday	4	FALSE
11/13/2019	2019	110	Wednesday	3	FALSE
11/12/2019	2019	102	Tuesday	2	FALSE
11/11/2019	2019	113	Monday	1	FALSE
11/10/2019	2019	3	Sunday	7	FALSE
11/9/2019	2019	4	Saturday	6	FALSE
11/8/2019	2019	99	Friday	5	FALSE
11/7/2019	2019	109	Thursday	4	FALSE
11/6/2019	2019	107	Wednesday	3	FALSE
11/5/2019	2019	108	Tuesday	2	FALSE
11/4/2019	2019	116	Monday	1	FALSE
11/3/2019	2019	1	Sunday	7	FALSE
11/2/2019	2019	2	Saturday	6	FALSE
11/1/2019	2019	115	Friday	5	FALSE
10/31/2019	2019	119	Thursday	4	FALSE
10/30/2019	2019	109	Wednesday	3	FALSE
10/29/2019	2019	114	Tuesday	2	FALSE
10/28/2019	2019	99	Monday	1	FALSE
10/27/2019	2019	2	Sunday	7	FALSE
10/26/2019	2019	2	Saturday	6	FALSE
10/25/2019	2019	78	Friday	5	FALSE
10/24/2019	2019	106	Thursday	4	FALSE
10/23/2019	2019	104	Wednesday	3	FALSE
10/22/2019	2019	115	Tuesday	2	FALSE
10/21/2019	2019	111	Monday	1	FALSE
10/20/2019	2019	2	Sunday	7	FALSE
10/19/2019	2019	3	Saturday	6	FALSE
10/18/2019	2019	69	Friday	5	FALSE
10/17/2019	2019	103	Thursday	4	FALSE
10/16/2019	2019	112	Wednesday	3	FALSE
10/15/2019	2019	116	Tuesday	2	FALSE
10/14/2019	2019	105	Monday	1	FALSE
10/13/2019	2019	5	Sunday	7	FALSE
10/12/2019	2019	4	Saturday	6	FALSE
10/11/2019	2019	104	Friday	5	FALSE
10/10/2019	2019	112	Thursday	4	FALSE
10/9/2019	2019	117	Wednesday	3	FALSE
10/8/2019	2019	118	Tuesday	2	FALSE
10/7/2019	2019	98	Monday	1	FALSE
10/6/2019	2019	4	Sunday	7	FALSE
10/5/2019	2019	6	Saturday	6	FALSE
10/4/2019	2019	101	Friday	5	FALSE
10/3/2019	2019	117	Thursday	4	FALSE
10/2/2019	2019	116	Wednesday	3	FALSE
10/1/2019	2019	108	Tuesday	2	FALSE

Door Swipes per Day (includes repeat Badge swipes)

October 1st, 2019 - December 31st, 2019

Notes: The summary below represents the "Door Swipe" events by personnel per building for each day in the Study Period. This data does not represent the total number of personnel visiting the buildings in a day; it counts the number of Door Swipes for each building which consists of multiple entries for a unique Employee Badge # as an employee enters and leaves buildings repeatedly. Thus, there are hundreds of door swipes per building in a day as personnel come and go. The intention of this dataset is to show the busiest day in the Study Period. Additional analysis is located in Appendix C which shows the headcount per hour for each building on the busiest day (worst case scenario). When referencing the full dataset in Appendix D, the Headquarters Building category represents the "KTM" building. The Motorsports Building represents the Husqvarna and Training buildings.

Date	Day	Headquarters Building	Motorsports Building	Total Swipes	Busiest Day?
10/1/2019	2	155	195	350	FALSE
10/2/2019	3	206	176	382	FALSE
10/3/2019	4	155	171	326	FALSE
10/4/2019	5	156	157	313	FALSE
10/5/2019	6	3	6	9	FALSE
10/6/2019	7	3	4	7	FALSE
10/7/2019	1	142	152	294	FALSE
10/8/2019	2	171	202	373	FALSE
10/9/2019	3	163	190	353	FALSE
10/10/2019	4	162	225	387	FALSE
10/11/2019	5	150	161	311	FALSE
10/12/2019	6	2	5	7	FALSE
10/13/2019	7	3	5	8	FALSE
10/14/2019	1	137	188	325	FALSE
10/15/2019	2	142	197	339	FALSE
10/16/2019	3	181	210	391	FALSE
10/17/2019	4	146	180	326	FALSE
10/18/2019	5	106	92	198	FALSE
10/19/2019	6	4	3	7	FALSE
10/20/2019	7	1	2	3	FALSE
10/21/2019	1	153	168	321	FALSE
10/22/2019	2	166	218	384	FALSE
10/23/2019	3	155	162	317	FALSE
10/24/2019	4	184	198	382	FALSE
10/25/2019	5	152	153	305	FALSE
10/26/2019	6	2	6	8	FALSE
10/27/2019	7	1	8	9	FALSE
10/28/2019	1	185	175	360	FALSE
10/29/2019	2	176	207	383	FALSE
10/30/2019	3	182	213	395	FALSE
10/31/2019	4	219	214	433	FALSE
11/1/2019	5	178	184	362	FALSE
11/2/2019	6	2	6	8	FALSE
11/3/2019	7	0	5	5	FALSE
11/4/2019	1	161	194	355	FALSE
11/5/2019	2	154	198	352	FALSE
11/6/2019	3	177	183	360	FALSE
11/7/2019	4	187	129	316	FALSE
11/8/2019	5	151	135	286	FALSE
11/9/2019	6	4	4	8	FALSE
11/10/2019	7	3	5	8	FALSE
11/11/2019	1	168	171	339	FALSE
11/12/2019	2	184	126	310	FALSE
11/13/2019	3	184	136	320	FALSE
11/14/2019	4	187	111	298	FALSE
11/15/2019	5	178	125	303	FALSE
11/16/2019	6	7	8	15	FALSE
11/17/2019	7	1	9	10	FALSE

11/18/2019	1	156	174	330	FALSE
11/19/2019	2	180	178	358	FALSE
11/20/2019	3	202	222	424	FALSE
11/21/2019	4	163	219	382	FALSE
11/22/2019	5	64	107	171	FALSE
11/23/2019	6	12	7	19	FALSE
11/24/2019	7	1	7	8	FALSE
11/25/2019	1	152	227	379	FALSE
11/26/2019	2	160	220	380	FALSE
11/27/2019	3	113	159	272	FALSE
11/28/2019	4	0	3	3	FALSE
11/29/2019	5	9	15	24	FALSE
11/30/2019	6	4	8	12	FALSE
12/1/2019	7	6	10	16	FALSE
12/2/2019	1	169	212	381	FALSE
12/3/2019	2	237	169	406	FALSE
12/4/2019	3	223	198	421	FALSE
12/5/2019	4	192	156	348	FALSE
12/6/2019	5	152	144	296	FALSE
12/7/2019	6	10	8	18	FALSE
12/8/2019	7	3	7	10	FALSE
12/9/2019	1	168	255	423	FALSE
12/10/2019	2	185	203	388	FALSE
12/11/2019	3	209	195	404	FALSE
12/12/2019	4	191	165	356	FALSE
12/13/2019	5	200	171	371	FALSE
12/14/2019	6	2	6	8	FALSE
12/15/2019	7	1	5	6	FALSE
12/16/2019	1	208	232	440	FALSE
12/17/2019	2	210	248	458	FALSE
12/18/2019	3	227	234	461	TRUE
12/19/2019	4	201	238	439	FALSE
12/20/2019	5	214	236	450	FALSE
12/21/2019	6	7	8	15	FALSE
12/22/2019	7	11	3	14	FALSE
12/23/2019	1	149	139	288	FALSE
12/24/2019	2	15	13	28	FALSE
12/25/2019	3	1	4	5	FALSE
12/26/2019	4	85	132	217	FALSE
12/27/2019	5	83	100	183	FALSE
12/28/2019	6	11	11	22	FALSE
12/29/2019	7	3	5	8	FALSE
12/30/2019	1	119	144	263	FALSE
12/31/2019	2	35	57	92	FALSE

Hourly Headcount by Building for Days with Over 120 Unique Badge Swipes

Notes: The summary below represents the number of personnel ("Headcount") per hour for each building on days which logged over 120 unique Badge Swipes. In total, there are This summary excludes duplicate Employee Badge #s when a unique Employee Badge # swiped into the same building within the same hour. This provides an accurate representation of the number of personnel in a given building for each hour of the day. When referencing the full dataset in Appendix D, the Headquarters Building category represents the "KTM" building. The Motorsports Building represents the Husqvarna and Training buildings.

12/17/2019 -- Worst Case Scenario

Date	Hour	Time	HEADQUARTERS	MOTORSPORTS	Total
12/17/2019	0	12:00 AM	0	0	0
12/17/2019	1	1 00 AM	0	0	0
12/17/2019	2	2:00 AM	0	0	0
12/17/2019	3	3 00 AM	0	0	0
12/17/2019	4	4:00 AM	0	0	0
12/17/2019	5	5 00 AM	0	0	0
12/17/2019	6	6:00 AM	0	0	0
12/17/2019	7	7 00 AM	0	0	0
12/17/2019	8	8:00 AM	1	1	2
12/17/2019	9	9 00 AM	5	9	14
12/17/2019	10	10:00 AM	25	29	54
12/17/2019	11	11:00 AM	36	32	68
12/17/2019	12	12:00 PM	22	20	42
12/17/2019	13	1 00 PM	10	18	28
12/17/2019	14	2:00 PM	12	17	29
12/17/2019	15	3 00 PM	21	23	44
12/17/2019	16	4:00 PM	29	27	56
12/17/2019	17	5 00 PM	17	25	42
12/17/2019	18	6:00 PM	14	25	39
12/17/2019	19	7 00 PM	7	15	22
12/17/2019	20	8:00 PM	8	5	13
12/17/2019	21	9 00 PM	2	2	4
12/17/2019	22	10:00 PM	1	0	1
12/17/2019	23	11 00 PM	0	0	0

12/18/2019

Date	Hour	Time	HEADQUARTERS	MOTORSPORTS	Total
12/18/2019	0	12:00 AM	0	1	1
12/18/2019	1	1 00 AM	0	0	0
12/18/2019	2	2:00 AM	0	0	0
12/18/2019	3	3 00 AM	0	0	0
12/18/2019	4	4:00 AM	0	1	1
12/18/2019	5	5 00 AM	0	0	0
12/18/2019	6	6:00 AM	0	0	0
12/18/2019	7	7 00 AM	0	0	0
12/18/2019	8	8:00 AM	2	2	4
12/18/2019	9	9 00 AM	9	9	18
12/18/2019	10	10:00 AM	35	29	64
12/18/2019	11	11 00 AM	30	28	58
12/18/2019	12	12:00 PM	22	28	50
12/18/2019	13	1 00 PM	14	19	33
12/18/2019	14	2:00 PM	17	19	36
12/18/2019	15	3:00 PM	19	25	44
12/18/2019	16	4:00 PM	20	23	43
12/18/2019	17	5:00 PM	25	20	45

12/18/2019	18	6:00 PM	15	16	31
12/18/2019	19	7:00 PM	12	8	20
12/18/2019	20	8:00 PM	6	3	9
12/18/2019	21	9:00 PM	0	3	3
12/18/2019	22	10:00 PM	1	0	1
12/18/2019	23	11:00 PM	0	0	0

12/19/2019

Date	Hour	Time	HEADQUARTERS	MOTORSPORTS	Total
12/19/2019	0	12:00 AM	0	1	1
12/19/2019	1	1:00 AM	0	0	0
12/19/2019	2	2:00 AM	0	0	0
12/19/2019	3	3:00 AM	0	0	0
12/19/2019	4	4:00 AM	0	0	0
12/19/2019	5	5:00 AM	0	0	0
12/19/2019	6	6:00 AM	0	0	0
12/19/2019	7	7:00 AM	0	0	0
12/19/2019	8	8:00 AM	1	2	3
12/19/2019	9	9:00 AM	7	9	16
12/19/2019	10	10:00 AM	28	23	51
12/19/2019	11	11:00 AM	32	31	63
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12/19/2019	14	2:00 PM	15	22	37
12/19/2019	15	3:00 PM	23	21	44
12/19/2019	16	4:00 PM	19	25	44
12/19/2019	17	5:00 PM	21	19	40
12/19/2019	18	6:00 PM	10	22	32
12/19/2019	19	7:00 PM	9	11	20
12/19/2019	20	8:00 PM	8	4	12
12/19/2019	21	9:00 PM	0	2	2
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12/12/2019

Date	Hour	Time	HEADQUARTERS	MOTORSPORTS	Total
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12/12/2019	7	7:00 AM	0	0	0
12/12/2019	8	8:00 AM	0	3	3
12/12/2019	9	9:00 AM	1	11	12
12/12/2019	10	10:00 AM	8	18	26
12/12/2019	11	11:00 AM	28	22	50
12/12/2019	12	12:00 PM	39	12	51
12/12/2019	13	1:00 PM	17	14	31
12/12/2019	14	2:00 PM	8	15	23
12/12/2019	15	3:00 PM	5	16	21
12/12/2019	16	4:00 PM	12	13	25
12/12/2019	17	5:00 PM	28	10	38
12/12/2019	18	6:00 PM	13	17	30
12/12/2019	19	7:00 PM	13	9	22
12/12/2019	20	8:00 PM	12	4	16

12/12/2019	21	9:00 PM	6	0	6
12/12/2019	22	10:00 PM	1	0	1
12/12/2019	23	11:00 PM	0	0	0

12/11/2019

Date	Hour	Time	HEADQUARTERS	MOTORSPORTS	Total
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12/11/2019	3	3:00 AM	0	0	0
12/11/2019	4	4:00 AM	0	0	0
12/11/2019	5	5:00 AM	0	0	0
12/11/2019	6	6:00 AM	0	0	0
12/11/2019	7	7:00 AM	0	1	1
12/11/2019	8	8:00 AM	3	3	6
12/11/2019	9	9:00 AM	8	8	16
12/11/2019	10	10:00 AM	32	20	52
12/11/2019	11	11:00 AM	41	25	66
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12/11/2019	14	2:00 PM	12	16	28
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12/10/2019

Date	Hour	Time	HEADQUARTERS	MOTORSPORTS	Total
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12/10/2019	4	4:00 AM	0	0	0
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12/10/2019	7	7:00 AM	0	1	1
12/10/2019	8	8:00 AM	2	1	3
12/10/2019	9	9:00 AM	6	14	20
12/10/2019	10	10:00 AM	24	22	46
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12/10/2019	12	12:00 PM	10	13	23
12/10/2019	13	1:00 PM	19	20	39
12/10/2019	14	2:00 PM	16	14	30
12/10/2019	15	3:00 PM	21	16	37
12/10/2019	16	4:00 PM	19	30	49
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12/10/2019	18	6:00 PM	11	17	28
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12/10/2019	20	8:00 PM	3	2	5
12/10/2019	21	9:00 PM	2	0	2
12/10/2019	22	10:00 PM	1	0	1
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12/9/2019

Date	Hour	Time	HEADQUARTERS	MOTORSPORTS	Total
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12/9/2019	2	2:00 AM	0	0	0
12/9/2019	3	3:00 AM	0	0	0
12/9/2019	4	4:00 AM	0	0	0
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12/9/2019	15	3:00 PM	22	30	52
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12/9/2019	17	5:00 PM	13	22	35
12/9/2019	18	6:00 PM	13	26	39
12/9/2019	19	7:00 PM	5	16	21
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12/4/2019

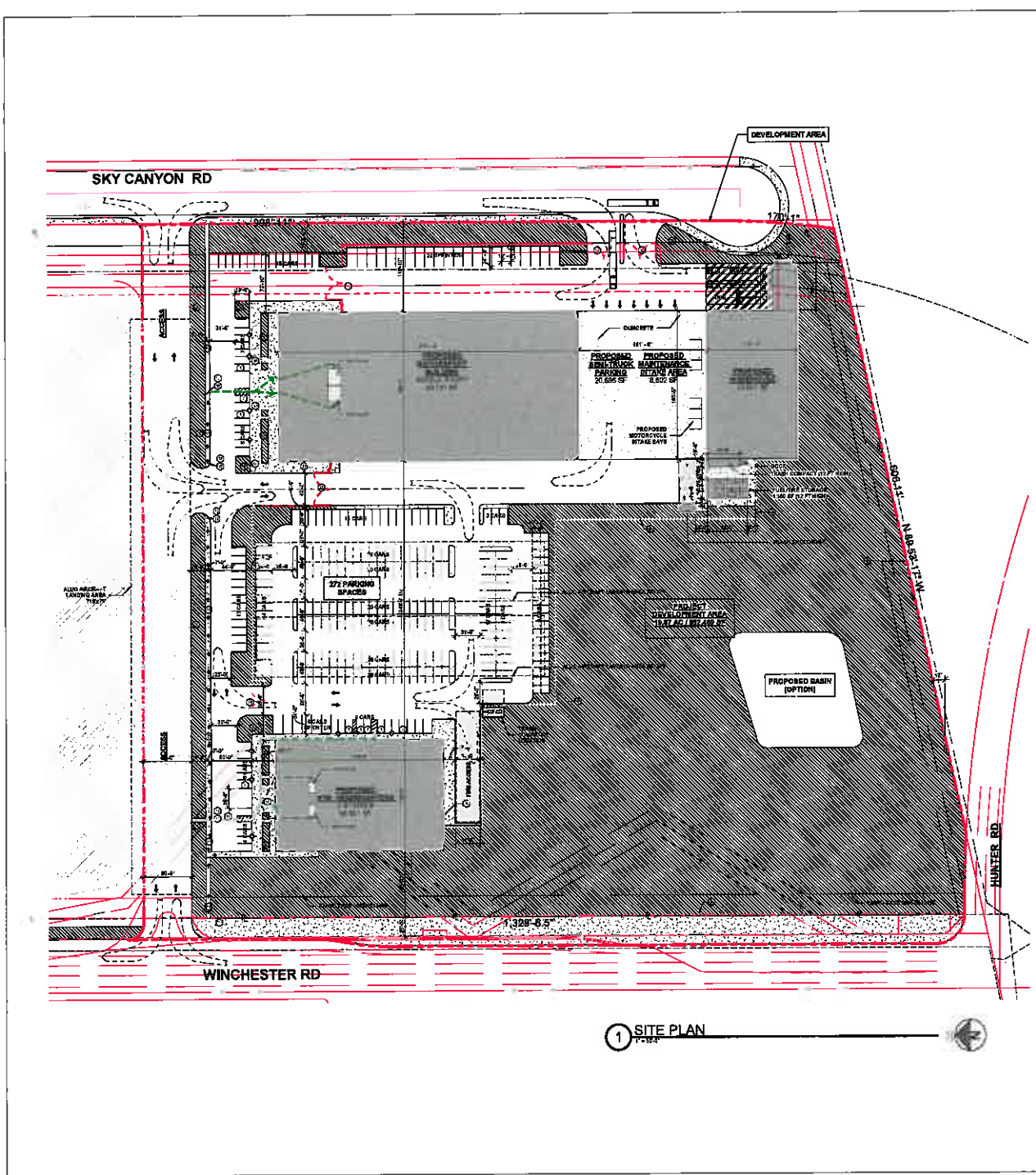
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12/4/2019	7	7:00 AM	0	1	1
12/4/2019	8	8:00 AM	3	1	4
12/4/2019	9	9:00 AM	12	11	23
12/4/2019	10	10:00 AM	29	21	50
12/4/2019	11	11:00 AM	29	21	50
12/4/2019	12	12:00 PM	12	20	32
12/4/2019	13	1:00 PM	10	11	21
12/4/2019	14	2:00 PM	12	10	22
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12/4/2019	18	6:00 PM	19	17	36
12/4/2019	19	7:00 PM	14	8	22
12/4/2019	20	8:00 PM	5	7	12
12/4/2019	21	9:00 PM	1	3	4
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12/3/2019

Date	Hour	Time	HEADQUARTERS	MOTORSPORTS	Total
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12/4/2019	5	5:00 AM	0	0	0
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12/4/2019	10	10:00 AM	36	22	58
12/4/2019	11	11:00 AM	39	12	51
12/4/2019	12	12:00 PM	14	10	24
12/4/2019	13	1:00 PM	36	19	55
12/4/2019	14	2:00 PM	16	11	27
12/4/2019	15	3:00 PM	18	18	36
12/4/2019	16	4:00 PM	24	22	46
12/4/2019	17	5:00 PM	18	19	37
12/4/2019	18	6:00 PM	15	14	29
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12/4/2019	20	8:00 PM	6	1	7
12/4/2019	21	9:00 PM	2	0	2
12/4/2019	22	10:00 PM	0	0	0
12/4/2019	23	11:00 PM	0	0	0

11/20/2019

Date	Hour	Time	HEADQUARTERS	MOTORSPORTS	Total
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11/20/2019	3	3:00 AM	0	0	0
11/20/2019	4	4:00 AM	0	1	1
11/20/2019	5	5:00 AM	0	0	0
11/20/2019	6	6:00 AM	0	0	0
11/20/2019	7	7:00 AM	0	1	1
11/20/2019	8	8:00 AM	0	2	2
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11/20/2019	10	10:00 AM	27	20	47
11/20/2019	11	11:00 AM	22	25	47
11/20/2019	12	12:00 PM	23	22	45
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11/20/2019	14	2:00 PM	15	12	27
11/20/2019	15	3:00 PM	27	20	47
11/20/2019	16	4:00 PM	25	27	52
11/20/2019	17	5:00 PM	14	19	33
11/20/2019	18	6:00 PM	8	21	29
11/20/2019	19	7:00 PM	10	8	18
11/20/2019	20	8:00 PM	6	6	12
11/20/2019	21	9:00 PM	0	5	5
11/20/2019	22	10:00 PM	1	1	2
11/20/2019	23	11:00 PM	0	0	0



1 SITE PLAN
1-1022

PROJECT DATA									
OWNER	WELLS BANCORP NORTH AMERICA LLC Trust Merica, CA 92522 T: 951-360-9807 F: 951-360-9804 www.wells.com								
SITE ADDRESS	Winchester Rd Murrieta, CA 92563								
ASSESSOR'S PARCEL NO.	026-013-001								
DESCRIPTION	THE SOUTH HALF OF GOVERNMENT LOT 2 EAST OF THE COUNTY ROAD IN THE SOUTHERN QUARTER OF SECTION 17, TOWNSHIP 3 SOUTH, RANGE 2 WEST, SAN BERNARDINO COUNTY, IN THE COUNTY OF SAN BERNARDINO, STATE OF CALIFORNIA, ACCORDING TO THE OFFICIAL RECORD.								
LAND USE INFORMATION AND OVERLAYS	<table border="0"> <tr> <td>SITE AREA</td> <td>PROPOSED SITE 184,488 SF</td> </tr> <tr> <td>SUBDIVISION</td> <td>ENTIRE 2.88 ACRES TRACT TO THE WEST VICINITY LOTS TO THE NORTH SOUTH AND REGION VALLEY APPLIC TO THE EAST</td> </tr> <tr> <td>ZONING</td> <td>VICINITY</td> </tr> <tr> <td>PROPOSED</td> <td>CO COMMERCIAL OFFICE</td> </tr> </table>	SITE AREA	PROPOSED SITE 184,488 SF	SUBDIVISION	ENTIRE 2.88 ACRES TRACT TO THE WEST VICINITY LOTS TO THE NORTH SOUTH AND REGION VALLEY APPLIC TO THE EAST	ZONING	VICINITY	PROPOSED	CO COMMERCIAL OFFICE
SITE AREA	PROPOSED SITE 184,488 SF								
SUBDIVISION	ENTIRE 2.88 ACRES TRACT TO THE WEST VICINITY LOTS TO THE NORTH SOUTH AND REGION VALLEY APPLIC TO THE EAST								
ZONING	VICINITY								
PROPOSED	CO COMMERCIAL OFFICE								
PRELIMINARY LOT COVERAGE	LOT COVERAGE: 18,338 SF (10.1%)								
LANDSCAPE	LANDSCAPE AREA: 88,822 SF								
PROJECT DESCRIPTION	CONSTRUCTION OF THREE NEW BUILDINGS ON AN ADJACENT LOT. BUILDING 1: 10,000 SF 1.5 STORY WITH OFFICES, CONFERENCE ROOMS, BREAK ROOMS, TRAVEL STORAGE & STORAGE SPACE. BUILDING 2: 10,000 SF 1.5 STORY WITH OFFICES, CONFERENCE ROOMS, BREAK ROOMS, STORAGE AND STORAGE SPACE. BUILDING 3: 10,000 SF 1.5 STORY WITH OFFICES, CONFERENCE ROOMS, BREAK ROOMS, STORAGE AND STORAGE SPACE. BUILDING 4: 10,000 SF 1.5 STORY WITH OFFICES, CONFERENCE ROOMS, BREAK ROOMS, STORAGE AND STORAGE SPACE.								
BUILDING AREA	<table border="0"> <tr> <td>BUILDING 1:</td> <td> <ul style="list-style-type: none"> 1ST FLOOR = 10,000 SF 2ND FLOOR = 10,000 SF TOTAL: 20,000 SF </td> </tr> <tr> <td>BUILDING 2:</td> <td> <ul style="list-style-type: none"> 1ST FLOOR = 10,000 SF 2ND FLOOR = 10,000 SF TOTAL: 20,000 SF </td> </tr> <tr> <td>BUILDING 3:</td> <td> <ul style="list-style-type: none"> 1ST FLOOR = 10,000 SF 2ND FLOOR = 10,000 SF TOTAL: 20,000 SF </td> </tr> </table>	BUILDING 1:	<ul style="list-style-type: none"> 1ST FLOOR = 10,000 SF 2ND FLOOR = 10,000 SF TOTAL: 20,000 SF 	BUILDING 2:	<ul style="list-style-type: none"> 1ST FLOOR = 10,000 SF 2ND FLOOR = 10,000 SF TOTAL: 20,000 SF 	BUILDING 3:	<ul style="list-style-type: none"> 1ST FLOOR = 10,000 SF 2ND FLOOR = 10,000 SF TOTAL: 20,000 SF 		
BUILDING 1:	<ul style="list-style-type: none"> 1ST FLOOR = 10,000 SF 2ND FLOOR = 10,000 SF TOTAL: 20,000 SF 								
BUILDING 2:	<ul style="list-style-type: none"> 1ST FLOOR = 10,000 SF 2ND FLOOR = 10,000 SF TOTAL: 20,000 SF 								
BUILDING 3:	<ul style="list-style-type: none"> 1ST FLOOR = 10,000 SF 2ND FLOOR = 10,000 SF TOTAL: 20,000 SF 								
TYPE OF CONSTRUCTION	NEW								
ADJACENT CLASSIFICATION	01, 02, 0, 0, 0								
PARKING	<table border="0"> <tr> <td>Required</td> <td>1000</td> <td>Required</td> <td>1000</td> </tr> <tr> <td>Net Available</td> <td>1000</td> <td>Net Available</td> <td>1000</td> </tr> </table>	Required	1000	Required	1000	Net Available	1000	Net Available	1000
Required	1000	Required	1000						
Net Available	1000	Net Available	1000						
VICINITY MAP									

KEY NOTES	
	AREA OF LANDSCAPE
	AREA OF PAVING
	DIRECTIONAL ARROW
	AREA OF FIRE LANE MINIMUM 20' WIDE
	180 DEGREE TRUCK TURNING RADIUS
	POLE MOUNT AREA LIGHT (10' HEAD) 100' HT. AT A.F.L.
	POLE MOUNT AREA LIGHT (10' HEAD) 80' HT. AT A.F.L.
	POLE MOUNT AREA LIGHT (10' HEAD) 60' HT. AT A.F.L.
	WALL MOUNT TYPE AREA FLOOD LIGHT - SEE ELECTRICAL ELEVATIONS FOR MOUNTING HEIGHT
	HANDICAP PATH OF TRAVEL, NOT TO EXCEED 5% SLOPE IN DIRECTION OF TRAVEL AND NOT TO EXCEED 1/8" CROSS SLOPE. USE APPROPRIATE WALL, SEE CIVIL DRAWINGS
	PROPOSED LOT LINE ADJUSTMENT
KEY NOTES	<ul style="list-style-type: none"> 1 24" x 12" PARKING STALLS 2 TRASH ENCLOSURE 3 LANDSCAPE 4 PROPERTY LINE 5 CONCRETE WALKWAY 6 LOADING AREA 7 ACCESSIBLE PARKING 8 PERIMETER AREA 9 PERIMETER WALL/FENCE 10 GATE 11 48" WIDE HANDICAP PATH OF TRAVEL, NOT TO EXCEED 5% SLOPE IN DIRECTION OF TRAVEL AND NOT TO EXCEED 1/8" CROSS SLOPE. USE APPROPRIATE WALL, SEE CIVIL DRAWINGS 12 DETECTABLE WARNING

architecture | planning | interior design

 240 N. Nevada St., Broomfield, CO 80020

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HOPBAUER | ARCHITECT

 P.L.L.C.

CASC

 COMMERCIAL ARCHITECTS & SCENARIOS

KTM

 KIM THONG CAMPUS

Winchester Rd,
 Murrieta, CA 92563

Plot Plan 180022

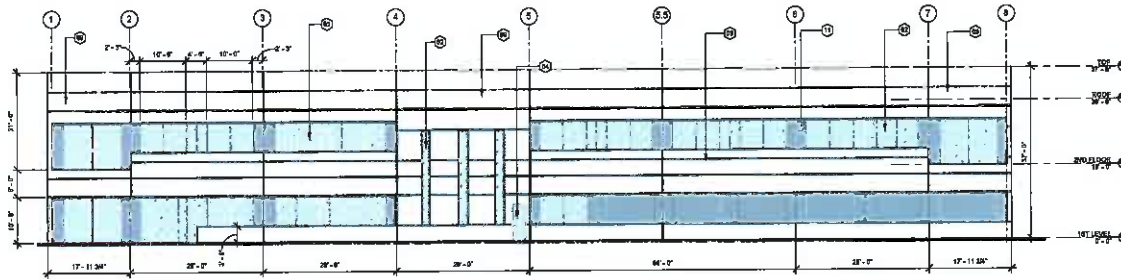
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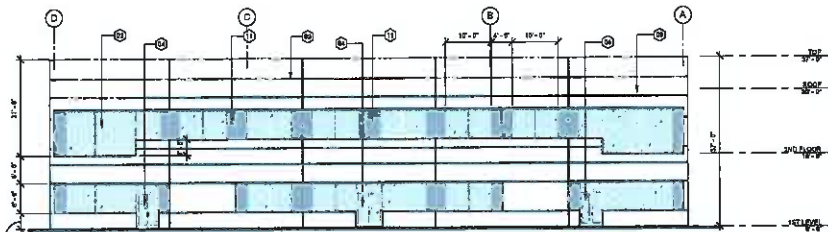
SITE PLAN

A1.1 G

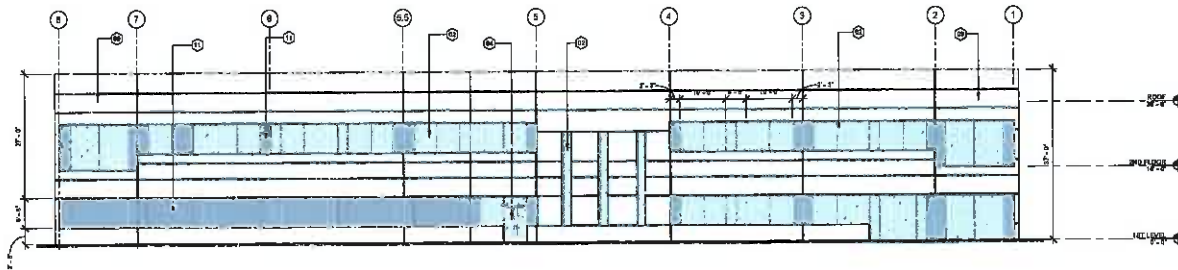
MARCA TECTIS architecture | planning | interior design



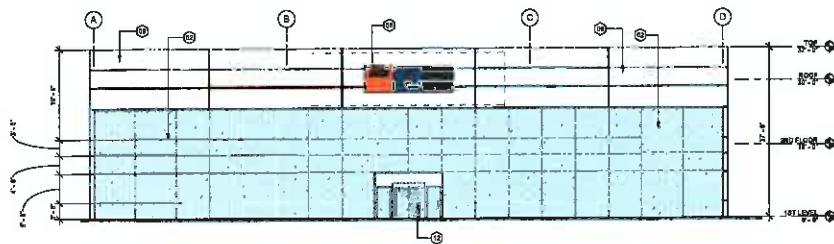
4 EXTERIOR ELEVATION - WEST



3 EXTERIOR ELEVATION - SOUTH



2 EXTERIOR ELEVATION - EAST



1 EXTERIOR ELEVATION - NORTH

LEGEND

KEY NOTES

02	STOREFRONT: ARCADIA SERIES DPG-3000, FRONT LOADED, CLEAR ANODIZED ALUMINUM GLAZING, INSULATED SOLARBAN 70XL CLEAR, TEMPERED
04	MAIN DOOR
05	ILLUMINATED SIGN 146 SF (NOT PART OF THIS SUBMITTAL)
06	CONCRETE TILT-UP
11	PROSTERSPANDREL
12	RECYCLING DOOR



HOPFAUER : ARCHITECT

CASC



Winchester Rd,
Murrieta, CA 92563



DATE	DESCRIPTION	NO.

PRINTED DATE: 1/15/2021 12:58:16 PM

STAMP



PROJECT NO.

DATE: 01/15/2021

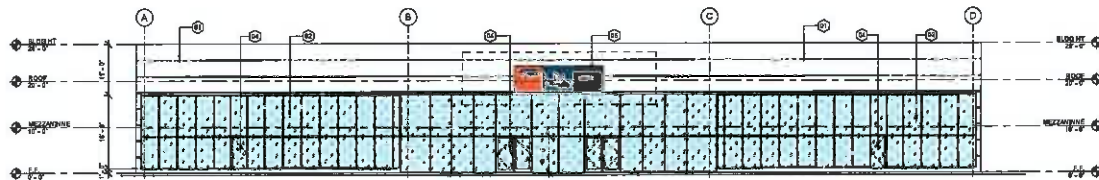
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EXTERIOR ELEVATIONS

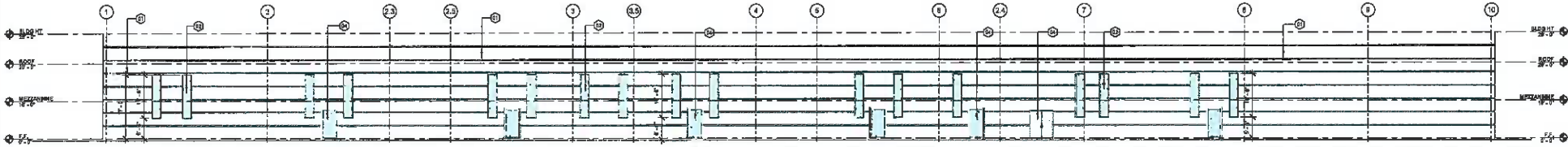
HEADQUARTERS BUILDING
SHEET
A2.1.1 HQ

MARCATECTS architecture | planning 2016 ©

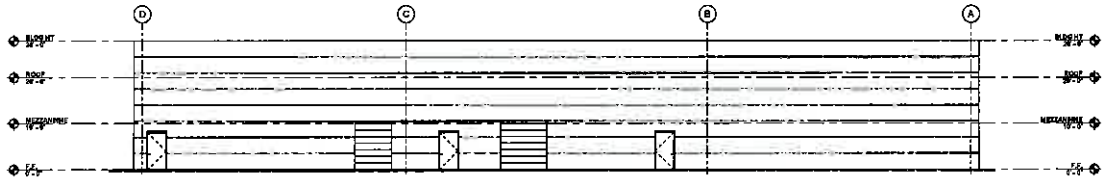
NOTE:
PARAPET HEIGHT TO BE
EQUAL OR GREATER THAN
THE HEIGHT OF ANY ROOF
MOUNTED ELEMENT.



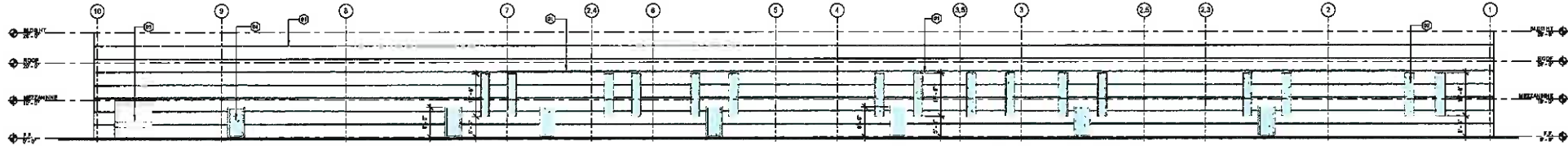
1 NORTH ELEVATION - MOTOR SPORTS



2 WEST ELEVATION - MOTOR SPORTS



3 SOUTH ELEVATION - MOTOR SPORTS



4 EAST ELEVATION - MOTOR SPORTS

NOTE:
PARAPET HEIGHT TO BE EQUAL OR GREATER THAN
THE HEIGHT OF ANY ROOF MOUNTED ELEMENT.

KEY NOTES

01	TELCUF
02	STICHES FRONT: ARCADIA SERIES CPO-3000 FRONT LOADED, CLEAR ANODIZED ALUMINUM GLAZING; INSULATED GLASS UNIT 70% CLEAR, TEMPERED COVER HEAD DOOR
03	COVER HEAD DOOR
04	MAN DOOR
05	ILLUMINATED SIGN 146 SF (NOT PART OF THIS SUBMITTAL)

DATE	DESCRIPTION	NO.

PRINTED DATE: 10/02/2011 10:29 AM

KTM

HOFBAUER ARCHITECT
21st Century Design

CASC
Engineering and Construction

MARCATECH
architecture | planning

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706.743.4100 | 706.743.4100 fax
marc@marcotech.com | www.marcotech.com

CLIENT: KTM AMERICA
PROJECT: KTM / MARC CAMPUS
ADDRESS: Winchester Rd, Murietta, CA 92563
PROJECT NO. 1718
STAMP:



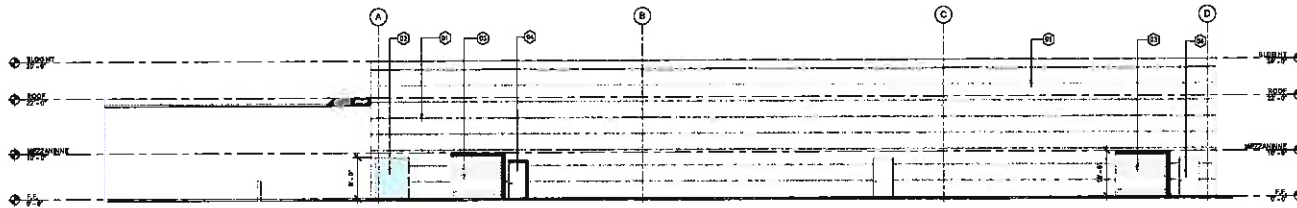
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EXTERIOR ELEVATIONS

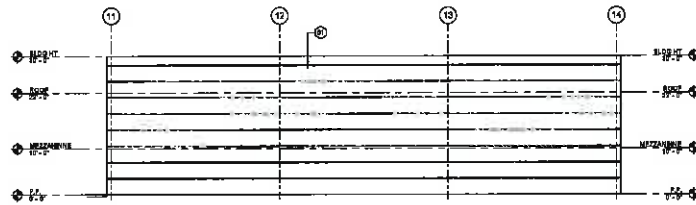
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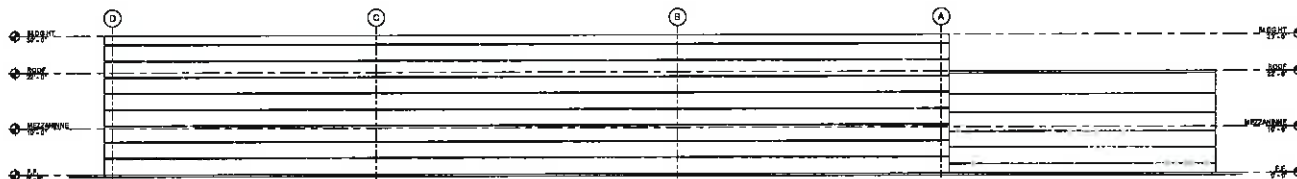
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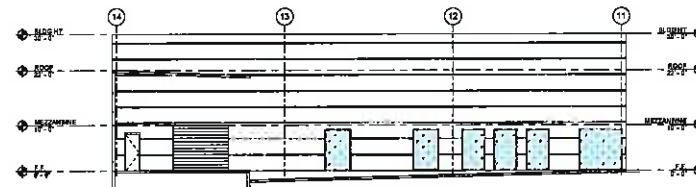
1 NORTH ELEVATION - WAREHOUSE



2 WEST ELEVATION - WAREHOUSE



3 SOUTH ELEVATION - WAREHOUSE



4 EAST ELEVATION - WAREHOUSE

NOTE:
PARAPET HEIGHT TO BE EQUAL OR GREATER THAN
THE HEIGHT OF ANY ROOF MOUNTED ELEMENT.

KEY NOTES		
01	TILT UP	
02	STOREFRONT: ARCADIA SERIES OFG-3500, FRONT LOADED, CLEAR ANODIZED ALUMINUM GLAZING, INSULATED POLYMER 70X, CLEAR, TEMPERED	
03	COVER HEAD DOOR	
04	MAN DOOR	
05	MANSORY TILTUP WALLS	
07	METAL ROOF COVERING	
08	TRASH ENCLOSURE	

DATE	DESCRIPTION	NO.

PRINTED DATE: 07/20/21 10:27:51 AM

HOFBAUER ARCHITECT
JIM HOFBAUER - OWNER

CASC
DR. & ARCHITECTURE LLC

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243 N. Market Pl., Emeryville, CA 94609
782.745.4100 | 782.745.4100 fax
marca@marc4cts.com | www.marc4cts.com

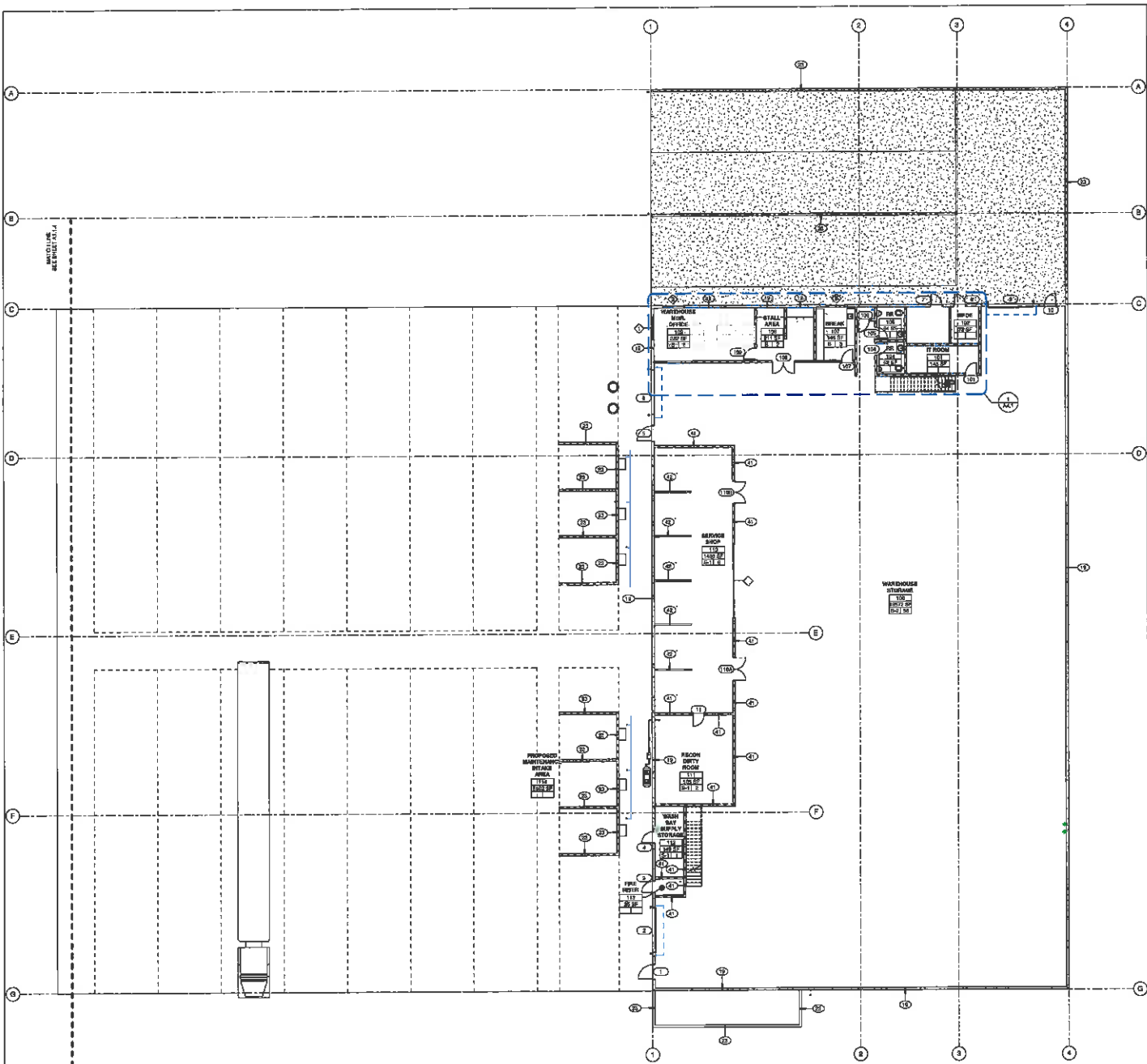
CLIENT	KTM AMERICA
PROJECT	KTM / MKO, CAMPUS
ADDRESS	Worcester Rd. Warren, CA 94583
PROJECT NO.	1710
STAMP	

DATE 01/20/21
SCALE 3/32" = 1'-0"
TITLE

EXTERIOR ELEVATIONS

WAREHOUSE BUILDING	BUILDING
STREET	
A2.1.2 WH	

MARCATECTS architecture | planning 2015 ©



1 1ST LEVEL OVERALL FLOOR PLAN (ANNOTATION)

WALL TYPE	WALL TYPE LEGEND	DESCRIPTION	DETAIL
11	11	EXTERIOR WALL - F-CONCRETE FIN UP WALL	11A.1
12	12	EXTERIOR WALL - F-ORG WALL	11A.2
13	13	EXTERIOR WALL - ONE LAYER SF TYPE 2 SYSTEMICALLY REINFORCED UP OF METAL FLUOR & EPIC	11A.3
14	14	EXTERIOR WALL - ONE LAYER SF TYPE 2 SYSTEMICALLY REINFORCED UP OF METAL FLUOR & EPIC	11A.4



Winchester Rd,
Murrieta, CA 92563



DATE	DESCRIPTION	NO.

PRINTED DATE: 4/15/21 4:55:58 PM

STAMP

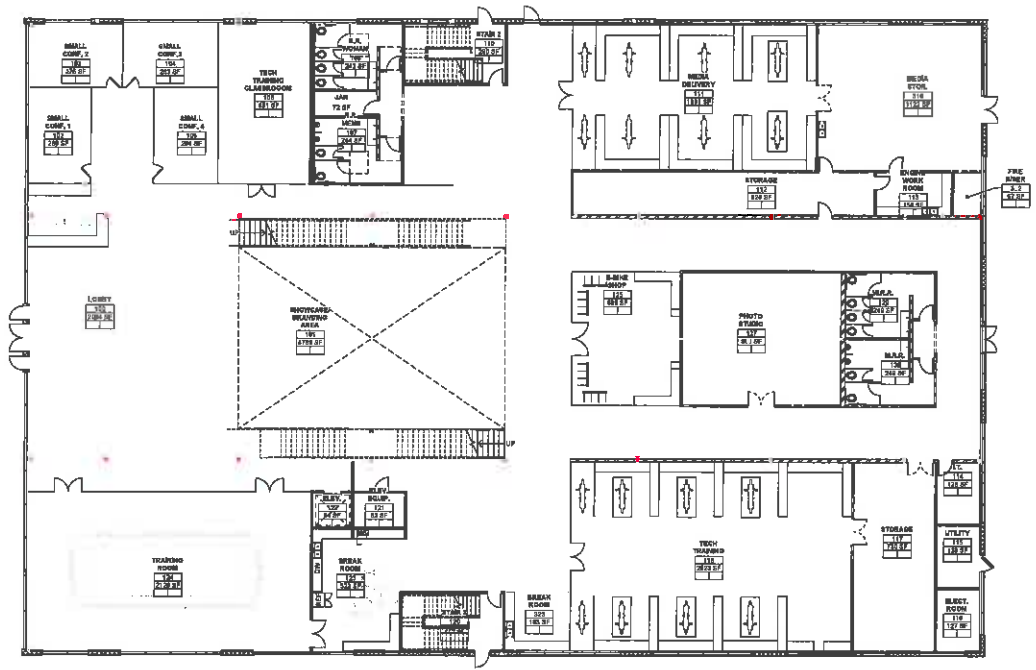
PROJECT NO.
DATE 4/15/2021
TITLE / SCALE 1/2" = 1'-0"

1ST LEVEL FLOOR PLAN (ANNOTATION)

SHEET NO. 11 OF 11
A1.2.0 WH



Winchester Rd,
 Murrieta, CA 92563



1 1ST LEVEL FLOOR PLAN

DATE	DESCRIPTION	NO.

PRINTED DATE: 04/02/2011 2:15:14 PM



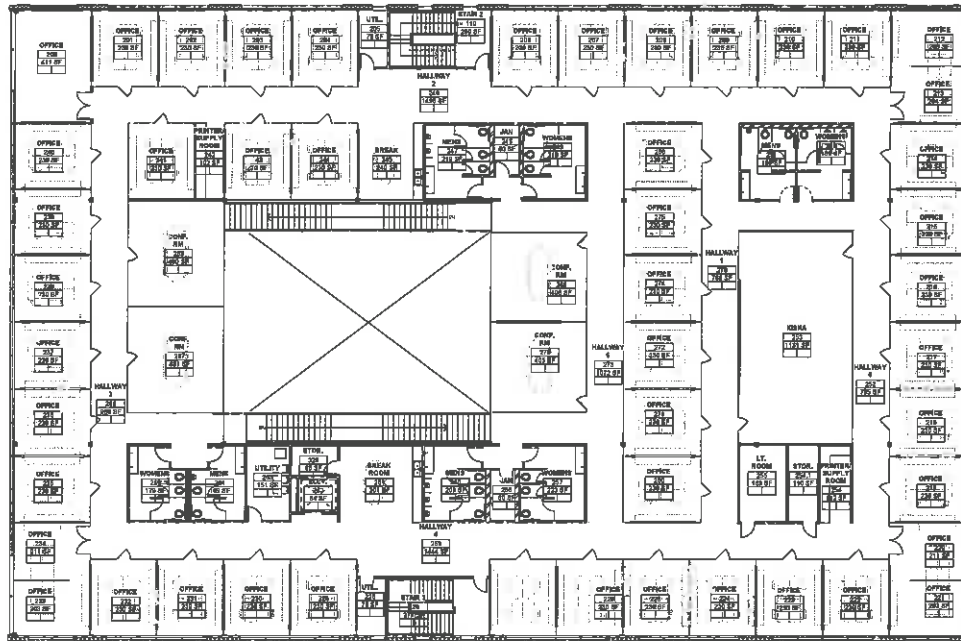
PROJECT NO.
 DATE 01/27/2011
 TITLE / SCALE 3/32" = 1'-0"

1ST LEVEL FLOOR PLAN

HEADQUARTERS BUILDING	BUILDING
SHEET	BUILDING
A1.1.3	HQ



Winchester Rd,
 Murrieta, CA 92563



1 2ND LEVEL FLOOR
 10'-0" = 1" = 0"

DATE	DESCRIPTION	NO.

PRINTED DATE: 01/22/2021 10:42 PM

STAMP



PROJECT NO.

DATE 01/22/2021

TITLE / SCALE 3/32" = 1'-0"

**2ND LEVEL FLOOR
 PLAN**

HEADQUARTERS BUILDING	BUILDING
SHEET	
A1.2.3	HQ

MARGATECHS architecture | planning 2018 ©



① 1ST LEVEL FLOOR PLAN _M.S. BUILDING
SHEET #10

DATE	DESCRIPTION	NO.

PRINTED DATE: 01/12/2011 12:42:31 PM



CLIENT: **KTM AMERICA**
 PROJECT: **KTM / HSOL CAMPUS**
 ADDRESS: **Winchester Rd, Murfreesboro, TN 37132**
 PROJECT NO.: **1719**
 STAMP: [Professional Engineer Seal]

DATE: **01/27/2011**
 SCALE: **3/32" = 1'-0"**
 TITLE: **1ST LEVEL FLOOR PLAN _M.S. BUILDING**
 SHEET: **A1.1.3 MS**
 PROJECT: **NOTICE REPORT BUILDING**
 MARCATECHS architects | engineering | planning 2010 ©

PROJECT DESCRIPTION

The Project is located within the French Valley community in unincorporated Riverside County on a 31.86 acre parcel identified as APN 963-030-002. The French Valley community consists of residential tract housing, ancillary shops and retail establishments, and a general aviation airport owned and operated by the County of Riverside. The Project is specifically located easterly of Highway 79 (SR-79), otherwise known as Winchester Road and southerly of Sparkman Way. Borel Road borders the Project on a portion of the southerly boundary, and Sky Canyon Drive is located on the easterly boundary. The Project is adjacent to the French Valley Airport and is located within the French Valley Airport Influence Area.

The Project's development area is approximately 19.87 acres, or 852,469 square feet, on the 31.86 acre parcel. The proposed development will provide 150,789 s.f. commercial office, warehousing, storage, research and development, and covered truck parking with a maintenance intake area on a 19.87 gross-acre site. The largest building will encompass approximately 68,741 square feet (s.f.) which will consist of KTM's motorsport research and development facility. A second building will be approximately 58,351 s.f., which will consist of the KTM headquarters. A third building will consist of a storage warehouse, which will be approximately 23,697 s.f. Additionally, approximately 20,696 s.f. is proposed for an open semi-truck parking area and approximately 8,602 s.f. is proposed for an open maintenance intake area to be adjacent to the Motorsport building. Adjacent to the Warehouse building is approximately 1,300 s.f. for open outdoor fuel/tire storage. Most of the proposed facility will be closed to the public and accommodate the administrative and operational aspects of the business. The development will provide infrastructure and public improvements necessary to support the development and is proposed to begin construction in 4th Quarter of 2021.

KTM HEADQUARTERS BUILDING (58,351 S.F.)

The Headquarters building will consist of general office functions such as sales and marketing, HR, accounting, graphic design and customer service, which are primarily done over phone and internet. Business hours will be Monday through Friday, (8:00am - 5:00pm). This facility will not be open to the public and will be closed to public access due to proprietary information.

MOTORSPORT BUILDING (68,741 S.F.) AND WAREHOUSE (23,697 S.F.) WITH ADJACENT SEMI-TRUCK PARKING (20,696 S.F.) AND MAINTENANCE INTAKE AREA (8,602 S.F.)

The Motorsport building serves as a training and research & development (R&D) facility. Dealer representatives are invited to the facility, where master technicians and mechanics disassemble the bikes by hand and re-build them by hand to show the dealer reps how to repair certain aspects of the bikes, as well as show case certain new functions of the bikes. These are all done for sales and marketing purposes so that the dealers can be experts and have a high level of knowledge over the product to convey to customers. There is no manufacturing or distribution in this facility. There are "Dyno Machines" used to bring the bikes up to speed and test their function.

The warehouse facility will function as a storage for KTM motocross bikes and trucks. The KTM Motorsport company brings in less than 1% of their total manufactured bikes to this facility. In short, the parts are manufactured in Europe, shipped over the Akron, OH where the motocross bikes are assembled. Then

they are all placed on trucks, and distributed to dealerships across North America (Canada, Mexico, USA). However, a small percent comes to this facility for testing, marketing, and research and development.

The truck parking area is approximately 20,696 s.f. and anticipates KTM-owned trucks 2-3 times per week during the racing season (3 months out of the year). The maintenance intake area is approximately 8,602 s.f. and will be used to maintain motocross bikes.

ALUC OPEN AREA FOR EMERGENCY LANDING OF SMALL PLANES (2.46 AC)

The Airport Land Use Commission (ALUC) reviews development within airport influence zones to ensure compliance with the Airport Land Use Compatibility Plan. The project site is located within French Valley Airport Influence Zone D and Zone B2. Zone D regulations require the project to dedicate a minimum of ten percent (10%) of the site within its zone as an "open area" for emergency landing of small planes. A total of 24.62 acres of the project site is located within Zone D; thus, the project is required to dedicate a minimum of 2.46 acres of open area within the project site. This area must have a minimum dimension of 75 feet by 300 feet and must be void of any obstacles or structures that are higher than four feet (4') in height or greater than four inches (4") in diameter. The proposed project meets this requirement by dedicating 2.46 acres of open area within the parking lot area and the driveway areas on the site.

NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893**. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The Riverside County Planning Department should be contacted on non-ALUC issues. For more information please contact Riverside County Planner Mr. Brett Dawson at (951) 955-0972.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to prull@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please telephone Barbara Santos at (951) 955-5132.

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside California

DATE OF HEARING: May 13, 2021

TIME OF HEARING: 9:30 A.M.

Pursuant to Executive Order N-25-20, this meeting will be conducted by teleconference and at the Place of Hearing, as listed above. Public access to the meeting location will be allowed, but limited to comply with the Executive Order. Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org

CASE DESCRIPTION:

ZAP1106FV21 – Pierer Immoreal North America, LLC (Representative: CASC Engineering and Consulting) – County of Riverside Case No. PPT180022S01 (Plot Plan Substantial Conformance), a proposal to construct a KTM motorsport research and development facility including a two-story 58,351 square foot headquarters office building, a 68,741 square foot motorsport research building, and a 23,697 square foot warehouse building (with an outdoor 20,696 square foot semi-truck parking area and an outdoor 8,602 square foot maintenance area) on 19.87 acres located easterly of Winchester Road, westerly of Sky Canyon Road, northerly of Borel Road, and Southerly of Sparkman Way (A proposal to establish a KTM motorsport facility and subsequent tentative parcel map had been previously found consistent by the ALUC) (Airport Compatibility Zones B2 and D of the French Valley Airport Influence Area).



RIVERSIDE COUNTY

AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: PPT180022S01

DATE SUBMITTED: 01/27/2021

F.V.
Zone B2
D x

APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

Applicant Pierer Immoreal North America, LLC (c/o Robin Grangruth) Phone Number (951) 600-8007 x4142
Mailing Address 38429 Innovation Court Email Robin.Grangruth@ktm.com
 Murrieta, CA 92563

Representative CASC Engineering and Consulting (c/o Frank Coyte, Planning Director) Phone Number (909) 783-0101 x5370
Mailing Address 1470 E. Cooley Drive Email FCoyle@cascinc.com
 Colton, CA 92324

Property Owner Pierer Immoreal North America, LLC (c/o John Hinz) Phone Number (951) 600-8007 x4142
Mailing Address 38429 Innovation Court Email Robin.Grangruth@ktm.com
 Murrieta, CA 92563

LOCAL JURISDICTION AGENCY

Local Agency Name County of Riverside Phone Number (951) 955 80972
Staff Contact BRETT DAWSON Email daalvarez@rivco.org
Mailing Address 4080 Lemon Street, 12th Floor Case Type PLOT PLAN
 P.O. Box 1409 General Plan / Specific Plan Amendment
 Riverside, CA 92502 Zoning Ordinance Amendment
 Subdivision Parcel Map / Tentative Tract
Local Agency Project No PPT180022 S01 Use Permit
 Site Plan Review/Plot Plan
 Other

PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address 37662 & 37862 Winchester Road
 Murrieta, CA
Assessor's Parcel No. 963-030-002 Gross Parcel Size 19.87
Subdivision Name N/A Nearest Airport and distance from Airport
Lot Number N/A French Valley - directly adjacent to site

PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe) The Project site is currently vacant. Existing Land Use designation is Commercial Office (CO) and Commercial Retail (CR) under the Borel Airpark Center Specific Plan. French Valley Airport is directly adjacent east of the Project site.

Proposed Land Use (describe)	The Project proposes a mixed-use development of three new buildings. Approximately 58,351 s.f. is proposed for office use; 68,741 s.f. is proposed for Motorsport use; and 23,697 s.f. for research & development. Additionally, 20,696 s.f. is proposed for open semi-truck parking and 8,602 s.f. is maintenance intake.		
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)	N/A	
For Other Land Uses (See Appendix C)	Hours of Operation	Business Hours: 8 a.m.- 5 p.m. for Headquarters building and R&D building	
	Number of People on Site	988	Maximum Number N/A
	Method of Calculation	Calculation based on California Building Code (see attached spreadsheet)	
Height Data	Site Elevation (above mean sea level)	1,325	ft.
	Height of buildings or structures (from the ground)	40	ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?		<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
	If yes, describe	N/A	

- A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:**
1. Completed ALUC Application Form
 1. ALUC fee payment
 1. Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
 1. Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
 1. CD with digital files of the plans (pdf)
 1. Vicinity Map (8.5x11)
 1. Detailed project description
 1. Local jurisdiction project transmittal
 3. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
 3. Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. **(Only required if the project is scheduled for a public hearing Commission meeting)**

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION
STAFF REPORT**

AGENDA ITEM: 3.3

HEARING DATE: May 13, 2021

CASE NUMBER: ZAP1457MA21 – Eugene Marini (Representative: Omega Engineering Consultants)

APPROVING JURISDICTION: City of Riverside

JURISDICTION CASE NO: PR-2021-001023 (General Plan Amendment, Rezone, Conditional Use Permit, Design Review)

LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

Airport Influence Area: March Air Reserve Base

Land Use Policy: Zone C2

Noise Levels: Below 60 CNEL contour

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends that the Commission find the proposed General Plan Amendment and Rezone CONSISTENT with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, and find the proposed Conditional Use Permit and Design Review CONSISTENT, subject to the conditions included herein.

PROJECT DESCRIPTION: The applicant proposes to construct a 3,590 square foot drive-thru carwash building on 0.89 acres. The applicant also proposes amending the site’s General Plan land use designation from Office (O) to Commercial (C), and changing the site’s zoning from Office (O) to Commercial General (CG).

PROJECT LOCATION: The site is located on the northwest corner of Mission Grove Parkway and Alessandro Boulevard within the jurisdiction of the City of Riverside, approximately 17,940 feet northwesterly of the northerly end of Runway 14-32 at March Air Reserve Base.

BACKGROUND:

Non-Residential Average Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zone C2, which limits average intensity to 200 people per acre.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan and the Additional Compatibility Policies included in the March ALUCP, the following rates were used to calculate the occupancy for the proposed project:

- Break Room – 1 person per 15 square feet
- Storage – 1 person per 300 square feet, and
- Office – 1 person per 200 square feet.

The project proposes to construct a 3,590 square foot drive-thru car wash, which includes 144 square feet of break room area, 748 square feet of storage area, 156 square feet of office area, and 25 car stacking drive-thru spaces, accommodating a total occupancy of 52 people, resulting in an average intensity of 58 people per acre, which is consistent with the Compatibility Zone C2 criterion of 200.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle). Based on the number of parking spaces provided (20 parking spaces, 25 car stacking drive-thru space) the total occupancy would be estimated at 68 people for an average intensity of 76 people per acre, which is consistent with the Compatibility Zone C2 criterion of 200.

Non-Residential Single-Acre Intensity: Compatibility Zone C2 limits maximum single-acre intensity to 500 people. There are no risk-reduction design bonuses available, as March Air Reserve Base/Inland Port Airport is primarily utilized by large aircraft weighing more than 12,500 pounds.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre area would include the entire site, consisting of 144 square feet of break room area, 748 square feet of storage area, and 156 square feet of office area, and 25 car stacking drive-thru spaces, resulting in a single acre occupancy of 52 people, which is consistent with the Compatibility Zone C2 single acre criterion of 500.

Prohibited and Discouraged Uses: The applicant does not propose any uses prohibited or discouraged in Compatibility Zone C2.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being below the 60 CNEL range from aircraft noise. Therefore, no special measures are required to mitigate aircraft-generated noise.

Part 77: The elevation of Runway 14-32 at its northerly terminus is 1,535 feet above mean sea level (1,535 feet AMSL). At a distance of approximately 17,940 feet from the project to the nearest point on the runway, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,714 feet AMSL. The site's elevation is 1,604 feet AMSL, and the proposed maximum building height is 29 feet, resulting in a top point elevation of 1,633 feet AMSL. Therefore, review by the FAA OES is not required.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

General Plan Amendment/Rezone: The applicant proposes amending the site's General Plan land use designation from Office (O) to Commercial (C), and changing the site's zoning from Office (O) to Commercial General (CG). The proposed amendments would be consistent with the Compatibility Plan as long as the underlying development's intensity is consistent with the compatibility criteria.

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use or activity which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use or activity which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Highly noise-sensitive outdoor nonresidential uses. Examples of noise-sensitive outdoor nonresidential uses that are prohibited include, but are not limited to, major spectator-oriented sports stadiums, amphitheaters, concert halls and drive-in theaters.
 - (f) Hazards to flight.
3. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property and be recorded as a deed notice. In the event that the Office

of the Riverside County Assessor-Clerk-Recorder declines to record said notice, the text of the notice shall be included on the Environmental Constraint Sheet (ECS) of the final parcel map, if an ECS is otherwise required.

4. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the stormwater basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the stormwater basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the stormwater basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin

5. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
6. The project has been evaluated for 3,590 square foot drive-thru car wash, which includes 144 square feet of break room area, 748 square feet of storage area, 156 square feet of office area, and 25 car stacking drive-thru spaces. Any increase in building area, or change in use to any higher intensity use, will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.
7. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission and March Air Reserve Base.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

**THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS**

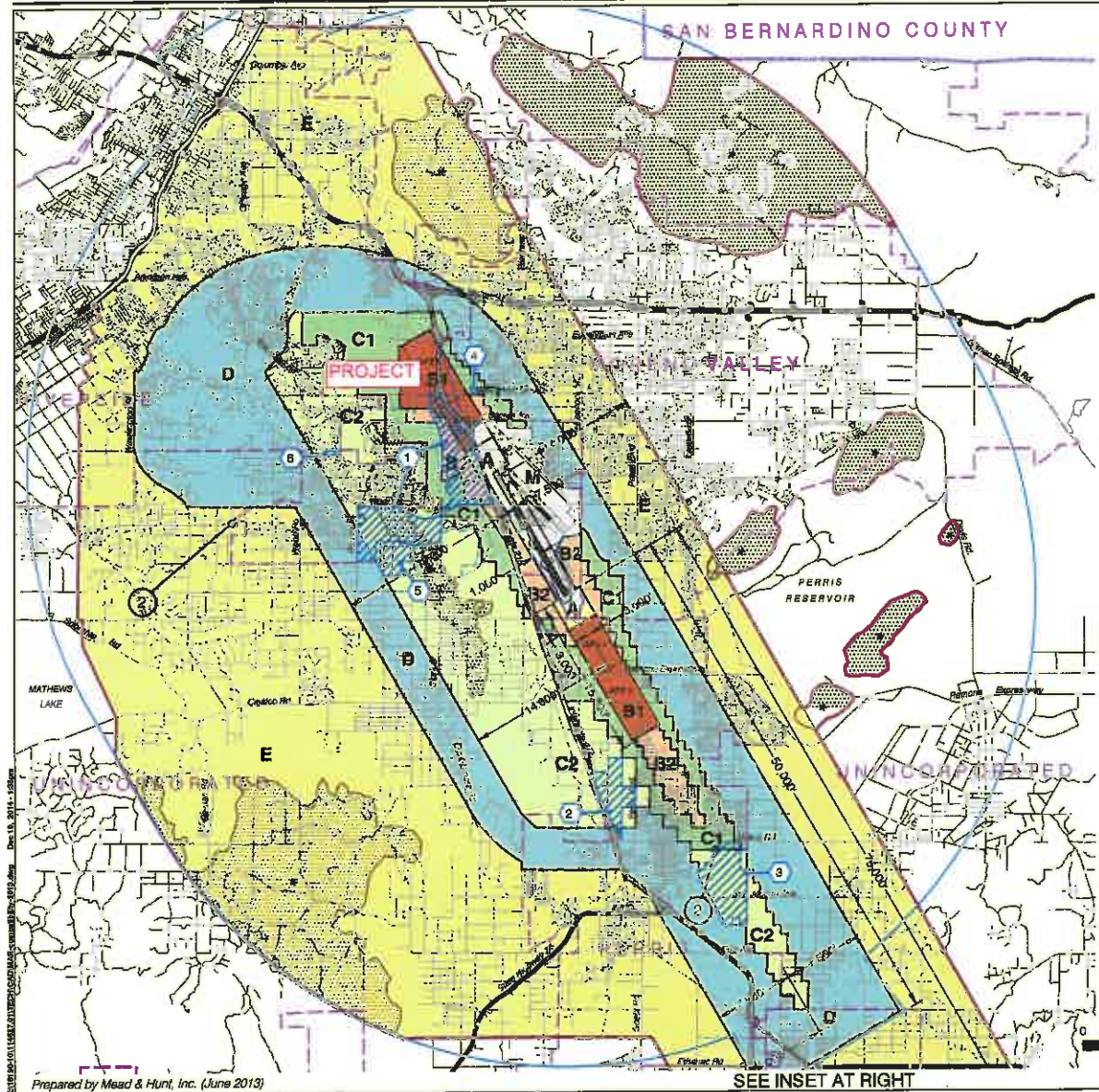
**PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES**



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



LEGEND

Compatibility Zones

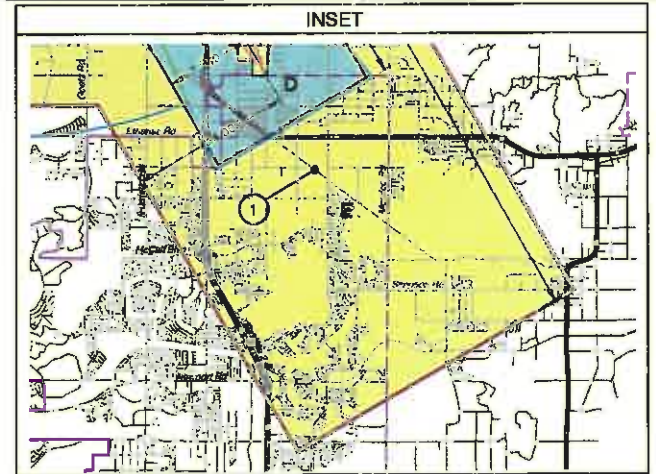
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Fidge Crest Subdivision



**Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)**

Note:
All dimensions are measured from runway ends and centerlines.



0 2 4 Miles

Base map source: County of Riverside 2013

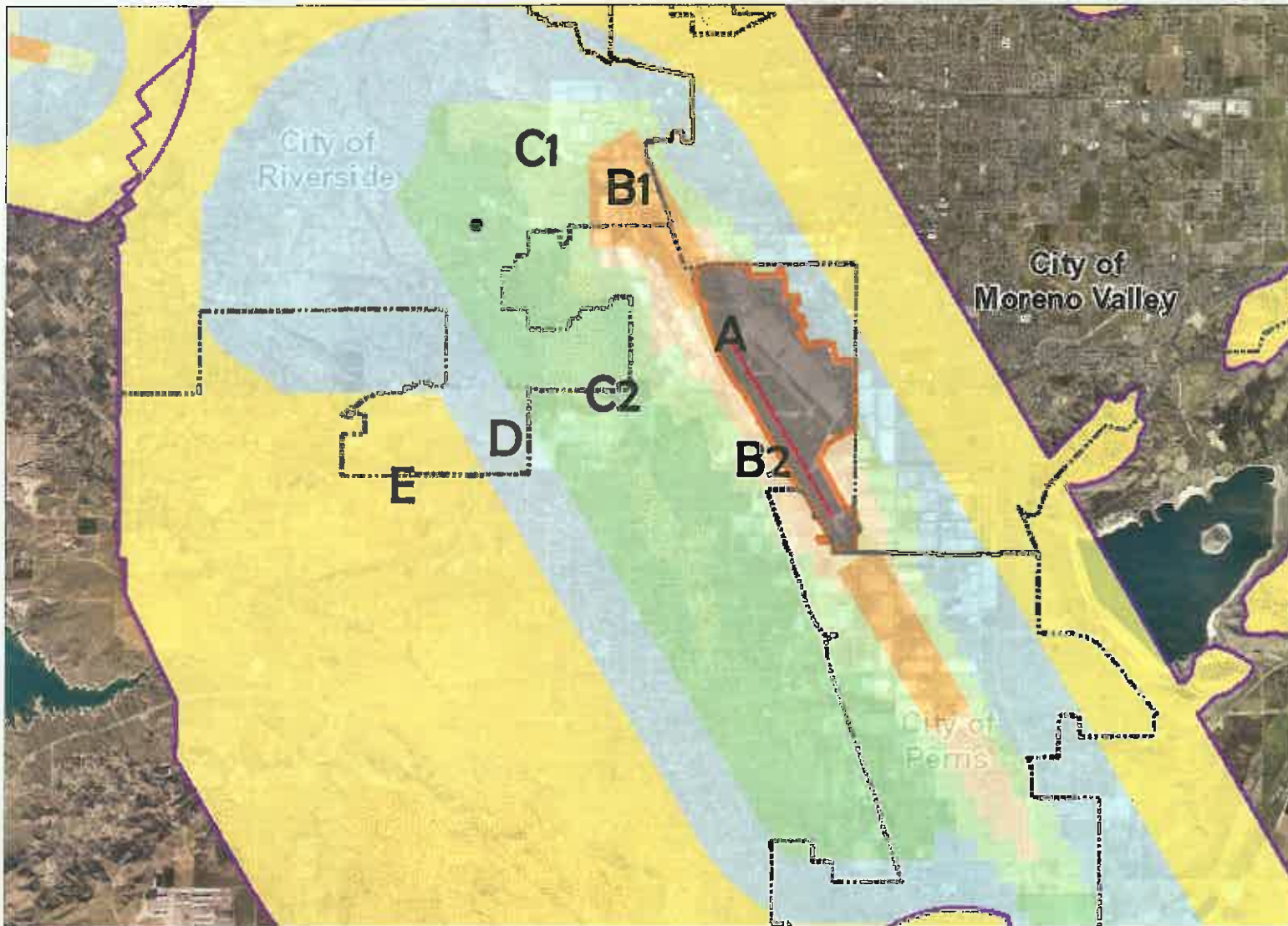
SEE INSET AT RIGHT

Prepared by Mead & Hunt, Inc. (June 2013)

Map MA-1

Compatibility Map
March Air Reserve Base / Inland Port Airport

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6

IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.



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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas

Airport Compatibility Zones

- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5

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Notes

Map My County Map



- Legend**
-  Parcels
 -  County Centerline Names
 -  County Centerlines
 -  Blueline Streams
 -  City Areas
 -  World Street Map

IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.



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Notes

Map My County Map



Legend

- County Centerlines
- Blue line Streams
- City Areas
- World Street Map



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Notes

Map My County Map



Legend

-  Parcels
-  County Centerlines
-  Blueline Streams
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Notes

Map My County Map



Legend

-  Parcels
-  County Centerline Names
-  County Centerlines
-  Blueline Streams
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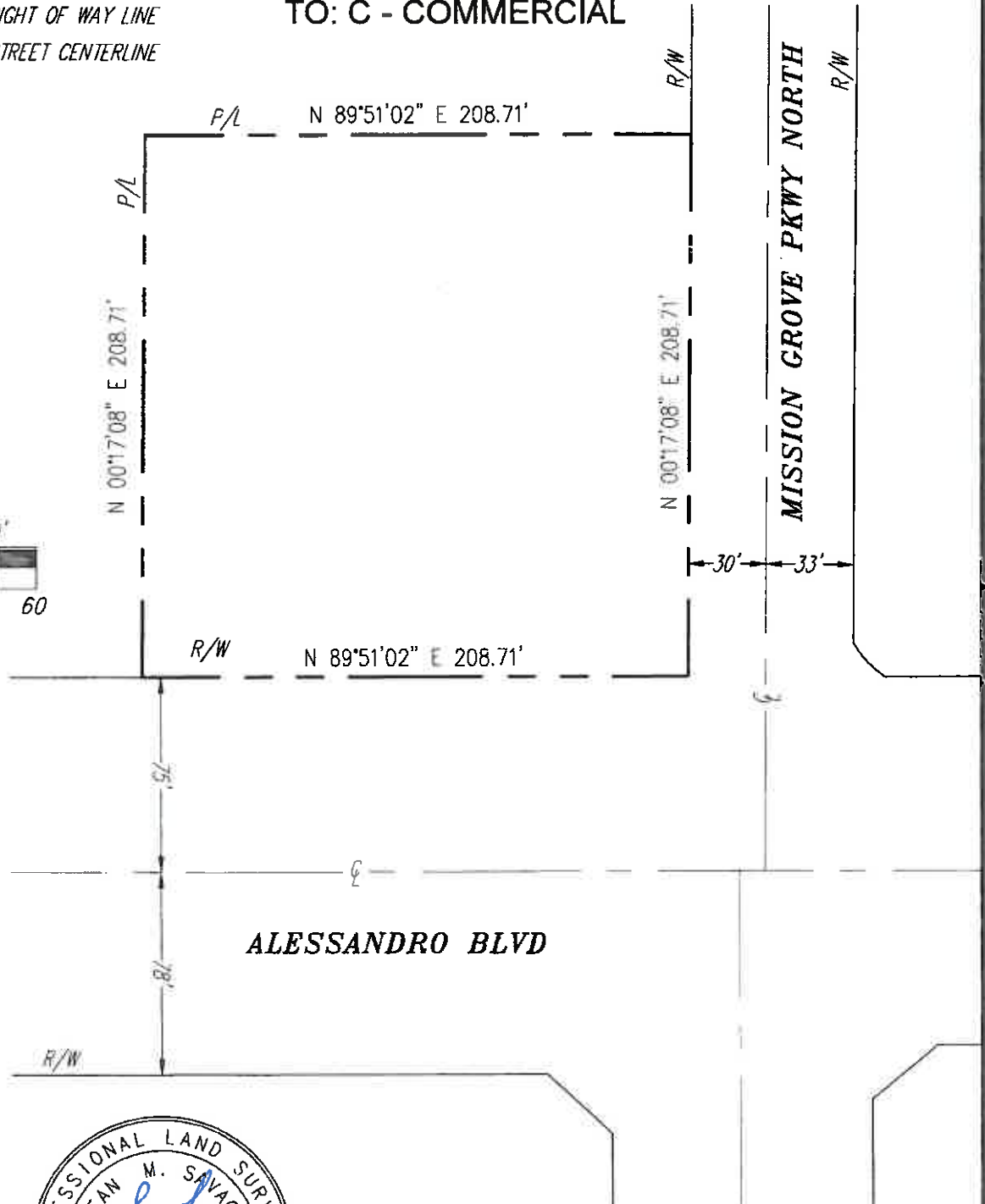
Notes

LEGEND:

-  PROJECT BOUNDARY
-  RIGHT OF WAY LINE
-  STREET CENTERLINE

**GENERAL PLAN AMENDMENT
PLAT
TO: C - COMMERCIAL**

From Office to Commercial



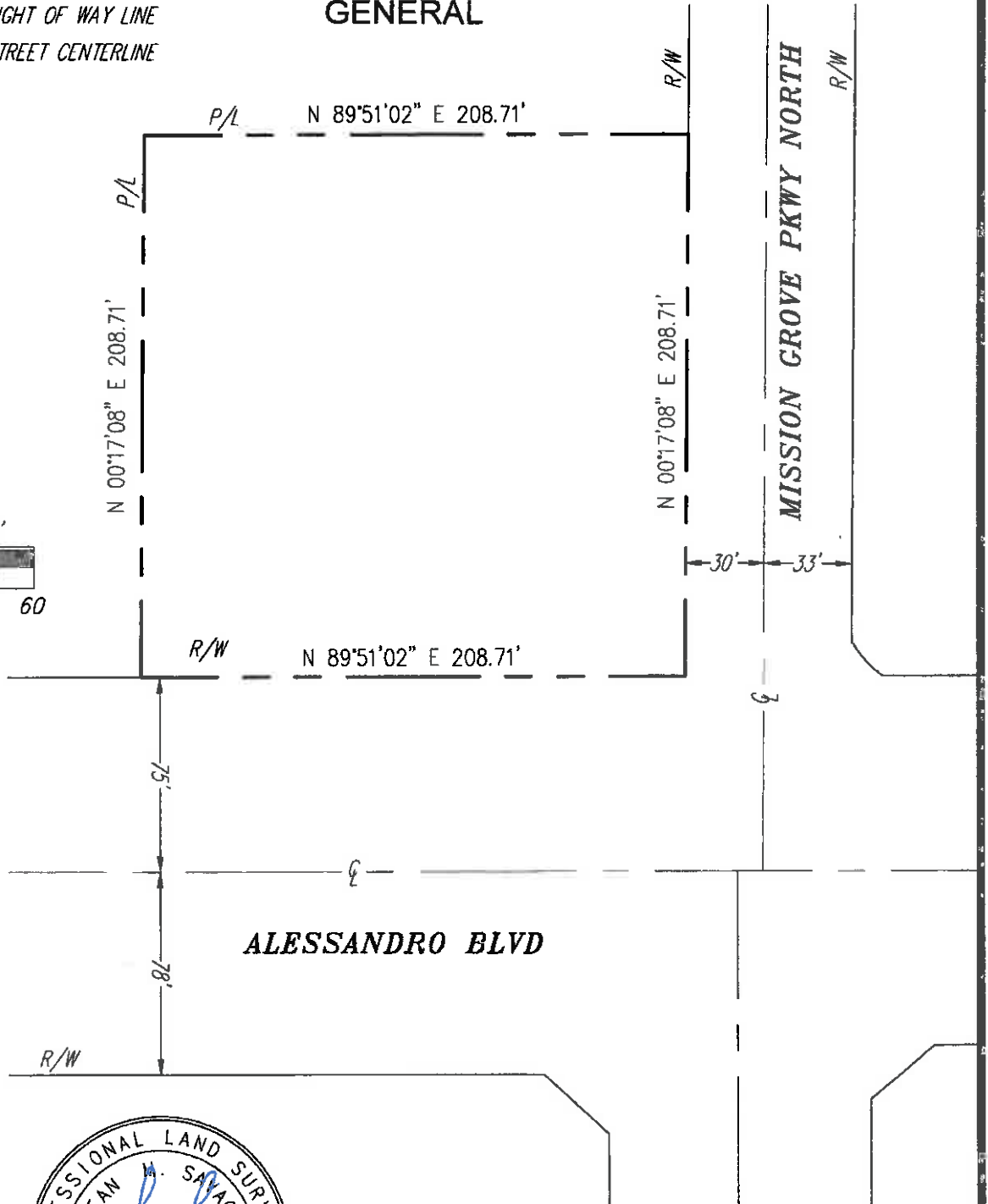
ΩMEGA
ENGINEERING CONSULTANTS
 4340 VIEWRIDGE AVE. SUITE B
 SAN DIEGO, CA 92123
 PH: (858) 634-8620 FAX: (858)-634-8627

LEGEND:

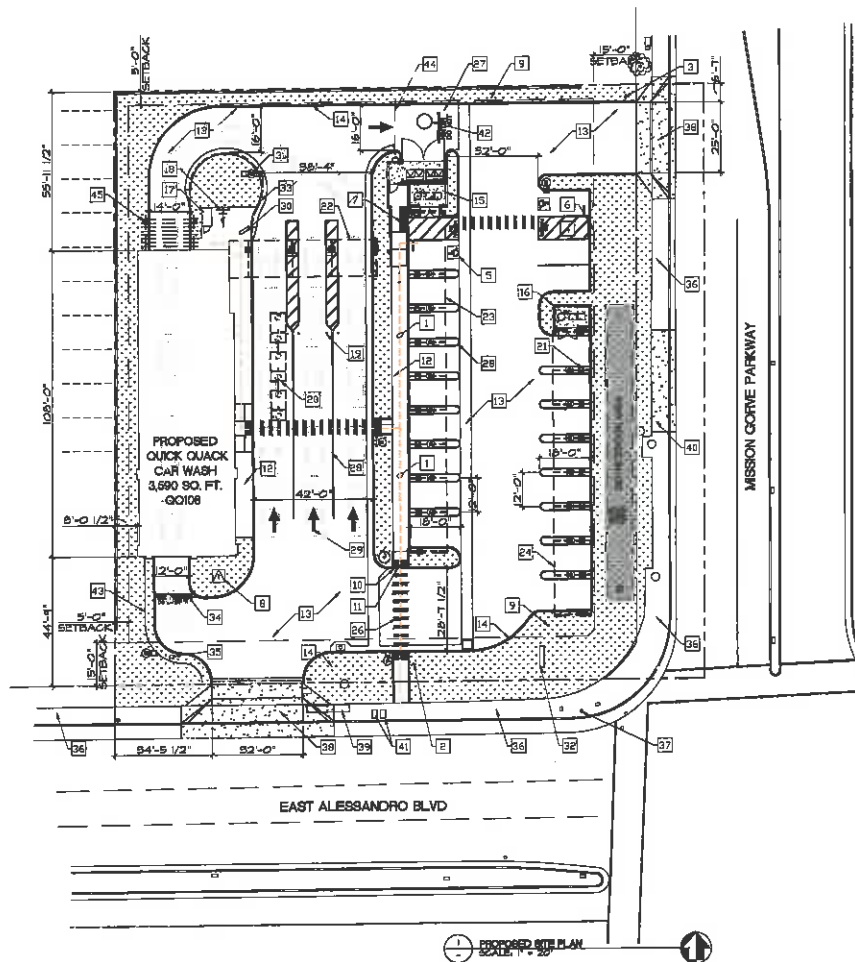
- PROJECT BOUNDARY
- RIGHT OF WAY LINE
- C--- STREET CENTERLINE

**CHANGE OF ZONE PLAT
TO: CG - COMMERCIAL
GENERAL**

**From Office to
Commercial General**



OMEGA
ENGINEERING CONSULTANTS
4340 VIEWRIDGE AVE. SUITE B
SAN DIEGO, CA 92123
PH: (858) 634-8620 FAX: (858)-634-8627



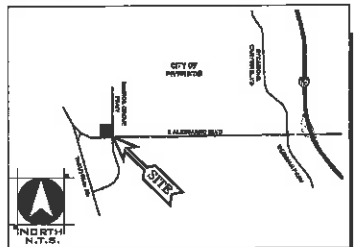
PROPOSED SITE PLAN
SCALE: 1" = 30'

SITE NUMBER	4-202
BUILDING	00-108
SITE AREA	53,556
STACKING LANES	3 LANES
STACKING WIDTH	42'-0"
VACUUM STALLS (1" x 12'-0" x 18'-0")	17 STALLS
VACUUM PRODUCERS	2 - 40HP
ADA PARKING	1 - 12'-0" x 18'-0"
PARKING STALLS	2 - 8' x 18'-0"
BUILDING AREA	3,546 S.F.
BUILDING SPRINKLED	NO
CB STATION ATTACHED	YES
BUILDING LENGTH	108'-0"
BACKROOM LENGTH	108'-0"
CONVEYOR TRENCH LENGTH	108'-0"
CONVEYOR LENGTH	102'-10"
CONVEYOR TYPE	SEAR / WHEEL PUSH
SIGNATURE	
APPROVAL DATE	

PARKING ANALYSIS

NEW 8'-0" X 11'-0" STANDARD STALL	2 STALLS
NEW 12'-0" X 13'-0" ADA STALL	1 STALL
NEW 12'-0" X 18'-0" VACUUM STALLS	17 STALLS
TOTAL STALLS	20 STALLS
NEW BIKE PARKING SPACES	3 SPACES
NEW LONG TERM BIKE PARKING SPACES	1 SPACE

VICINITY MAP



KEYNOTES

- 1 ADA PATH OF TRAVEL SHOWN DASHED.
- 2 NEW POLE MOUNTED INTERNATIONAL SYMBOL OF ACCESSIBILITY AT ACCESSIBLE PATH OF TRAVEL.
- 3 NEW TOW AWAY ACCESSIBILITY PARKING SIGN AT EACH ENTRANCE (TYPICAL 2 PLACES)
- 4 NEW VAN ACCESSIBLE VACUUM STALL - PAINT ACCESSIBLE LOADING ZONE WITH 4" WIDE STRIPING WITH TWO COATS OF HIGHWAY BLUE PAVEMENT MARKING PAINT. PAINT THE WORDS "NO PARKING" IN 12" HIGH LETTERS WITHIN THE LOADING ZONE.
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- 6 NEW ACCESSIBLE PARKING SIGN (TYPICAL 2 PLACES)
- 7 NEW TRUNCATED DOME (TYPICAL 5 PLACES)
- 8 NEW ELECTRICAL TRANSFORMER. VERIFY EXACT LOCATION WITH ELECTRIC COMPANY
- 9 NEW LANDSCAPING - SEE LANDSCAPE DRAWINGS. BIO-RETENTION AREAS ARE SHADED - SEE CIVIL PLANS
- 10 NEW FLUSH SURFACE AT TRANSITION (TYPICAL)
- 11 30 FOOT FLAG POLE. STYLE: "MEMORIAL" UNCOMMON USA. INSTALL PER MANUFACTURER'S INSTALLATION GUIDELINES. FOR REFERENCE ONLY - UNDER SEPARATE BUILDING PERMIT
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- 16 NEW VACUUM EQUIPMENT ENCLOSURE. SEE DRAWING A6.1
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- 18 NEW BICYCLE RACK
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- 26 NEW HIGHWAY PAINT WHITE 12" SIDE PEDESTRIAN STRIPING AT 36" O.S. (TYPICAL 3 PLACES)
- 27 NEW HIGHWAY PAINT YELLOW 18" "DO NOT ENTER" (TYPICAL 2 PLACES)
- 28 NEW HIGHWAY PAINT YELLOW 4" STRIPING
- 29 NEW HIGHWAY PAINT YELLOW PAVEMENT MARKINGS (TYPICAL)
- 30 NEW DISPLAY/PRICE SIGN - REFERENCE ONLY UNDER SEPARATE PERMIT (TYPICAL 3 PLACES)
- 31 NEW LED SITE LIGHTS (TYPICAL 5 PLACES) SEE PHOTOMETRIC PLAN
- 32 NEW MONUMENT SIGN - REFERENCE ONLY UNDER SEPARATE PERMIT
- 33 24" ROLLED CONCRETE CURB
- 34 NEW TRENCH DRAIN
- 35 NEW "STOP/GO" SIGN
- 36 EXISTING PUBLIC CONCRETE CURB, AND GUTTER TO REMAIN (REPAIR AS REQUIRED BY PUBLIC WORKS)
- 37 EXISTING STOP LIGHT TO REMAIN
- 38 NEW DRIVE APPROACH - SEE CIVIL DRAWINGS
- 39 EXISTING BENCH STOP TO BE RELOCATED
- 40 NEW CONCRETE SIDEWALK - SEE CIVIL DRAWINGS
- 41 NEW WATER METER - SEE CIVIL DRAWINGS
- 42 SUMP PUMP AND CONTROLLER - SEE CIVIL DRAWINGS
- 43 36" HIGH GREEN SCREEN
- 44 2 - 4" PIPE BOLLARDS AND CHAIN FOR EXIT ONLY
- 45 METAL TRELLIS ABOVE ENTRANCE



REVISION	DATE	DESCRIPTION
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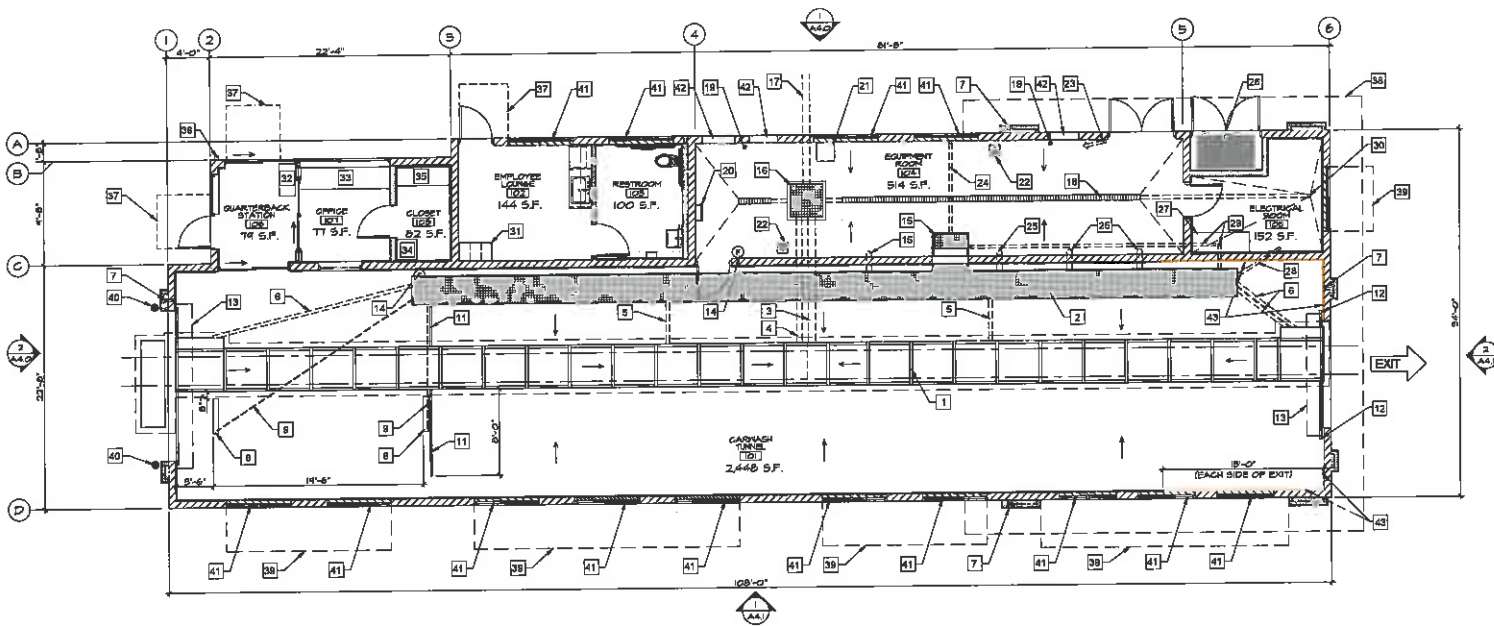
QUICK QUACK CAR WASH STORE 44-202
308 ALESSANDRO BLVD
RIVERSIDE, CA 92506

ARCHITECTURAL SITE PLAN

DATE	JANUARY 2021
CRM PROJECT #	2021



PLANNING DOCUMENTS



1 PROPOSED CAR WASH FLOOR PLAN
SCALE: 3/16" = 1'-0"

KEYNOTES

- 1 CONVEYOR TRENCH, SLOPE AT 1" PER FOOT TO DRAIN.
- 2 UTILITY TRENCH, SLOPE BOTTOM TO DRAIN.
- 3 8" PIPE FROM CONVEYOR TRENCH TO CLEAN OUT PIT.
- 4 6" OVERFLOW PIPE FROM CONVEYOR TRENCH TO CLEAN OUT PIT.
- 5 4" FLOOR DRAIN FROM UTILITY TRENCH TO THE CONVEYOR TRENCH SPACED EQUALLY IN UTILITY TRENCH (TYPICAL 2 PLACES)
- 6 4" PIPE CHASE FROM END OF UTILITY TRENCH TO CONVEYOR TRENCH. (TYPICAL 3 PLACES)
- 7 HOSE BIB IN RECESS BOX, PROVIDE WITH LOCKABLE DOOR AT EXTERIOR LOCATION. SEE PLUMBING DRAWINGS (TYPICAL 4 PLACES)
- 8 3/4" DEEP X 6" WIDE X 3'-5" LONG RECESS IN SLAB FOR TIRE SPINER SWITCH. (TYPICAL 2 PLACES)
- 9 1/2" CONDUIT AT END OF RECESS TO UTILITY TRENCH FOR TIRE SPINER. CONDUIT TO GO UNDER CONVEYOR TRENCH (TYPICAL 2 PLACES)
- 10 NOT USED
- 11 3/4" X 1 1/2" WIDE RECESS IN SLAB FOR DRAINAGE, SLOPE EACH SIDE TO CONVEYOR TRENCH
- 12 3/4" X 2 1/2" WIDE RECESS IN SLAB FOR DRAINAGE, SLOPE EACH SIDE TO CONVEYOR TRENCH, EXTEND 1'-0" PAST EXIT OPENING
- 13 COILING ROLL-UP DOOR (TYPICAL 2 PLACES)
- 14 WALL MOUNTED ENCLOSED FIRE EXTINGUISHER - 2A-10BC (TYPICAL 2 PLACES)
- 15 3'-4" WIDE X 2'-2" DEEP OPEN FOR ACCESS TO UTILITY TRENCH. CONTRACTOR TO PROVIDE COVER - MATCH UTILITY TRENCH
- 16 18'-0" X 9'-0" REGRAB CLEAN-OUT
- 17 8" PIPE FROM CLEAN OUT FIT TO GREASE INTERCEPTOR (GLANFIBR).
- 18 TRENCH DRAIN
- 19 ROOF DRAIN LEADER (TYPICAL 3 PLACES)
- 20 ROOF ACCESS LADDER
- 21 CONTRACTOR SUPPLIED AND INSTALLED EYE WASH STATION
- 22 18" X 18" X 12" DEEP FLOOR SINK, PLACE SINK AS CLOSE TO WALL AS POSSIBLE WITHOUT INTERFERING WITH FOOTING. (TYPICAL 2 PLACES)

- 23 1" PVC CHASE TO BUS JICE CONTROL PANEL
- 24 8" CHASE FROM ACCESS OPENING TO PAGE OF WALL
- 25 8" SWEEPS FROM UTILITY TRENCH TO EQUIPMENT ROOM (TYPICAL 4 PLACES)
- 26 ELECTRICAL SERVICE SWITCHGEAR
- 27 2'-0" X 5'-6" X 4" TALL CONCRETE 'HOUSE CLEANING PAD' FOR MCC PANEL
- 28 3" CHASE (NEXT TO CENTER PANEL) TO UTILITY TRENCH
- 29 2 - 8" CHASE AT CONTROL OF MCC PANEL 'HOUSE KEEPING PAD', RUN ONE CHASE TO EQUIPMENT TRENCH AND ONE NEXT TO ACCESS OPENING
- 30 4" FLOOR DRAIN - CONNECT TO TRENCH DRAIN
- 31 EMPLOYEE LOCKERS (TOTAL OF 5 - 1 ADA ACCESSIBLE) CONTRACTOR SUPPLIED AND INSTALLED
- 32 CONTRACTOR FURNISHED AND INSTALLED 18" X 36" X 40" TALL P.O.B. CABINET
- 33 3/4" MAXIMUM A.P.F. COUNTER TOP.
- 34 WALL MOUNTED IT CABINET
- 35 CONTRACTOR FURNISHED AND INSTALLED 20" X 34" X 80" TALL STORAGE CABINET WITH ADJUSTABLE SHELVING AND WHITE MELANINE FINISH ON THE INSIDE AND GRAY OUTSIDE
- 36 KNOX BOX - CONTRACTOR TO COORDINATE WITH LOCAL FIRE DEPARTMENT FOR TYPE, SIZE AND PREFERRED LOCATION
- 37 LEVEL LANDING PER CGC (BAG-2.3.4 AND TABLE (BAG-2.3.1) (TYP) - 24" MIN. STRENGTH CLEARANCE FOR THE EXTERIOR DOOR LANDING
- 38 LINE OF ROOF ABOVE.
- 39 WALL MOUNTED CANOPY ABOVE (TYPICAL 5 PLACES)
- 40 4" CONCRETE FILLED PIPE BOLLARD AT ENTRANCE (TYPICAL 2 PLACES)
- 41 PAIR FINISH - SEE EXTERIOR ELEVATIONS
- 42 30" WIDE X 18" HIGH AIR VENT - SEE EXTERIOR ELEVATIONS (TYP. 3 PLACES)
- 43 ADJUSTIBLOCK 'QUIET FIBER' PANELS

NOTE:
ALL CONDUITS TO HAVE SWEEPS ONLY. 90 DEGREE CONDUIT/GRADE BENDS ARE NOT ALLOWED



Quick Quack CAR WASH

REVISIONS

NO.	DATE	DESCRIPTION
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**QUICK QUACK CAR WASH
STORE #4-202
308 ALLESBENDRO BLVD
RIVERSIDE, CA 92508**

FLOOR PLAN

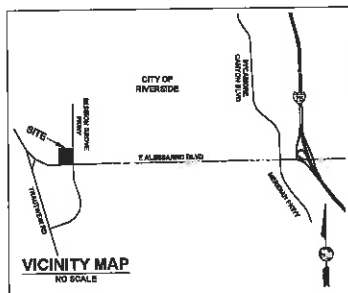
DATE: JANUARY 2001
CEN PROJECT #: 2000

ALL CONTRACTORS SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE CITY OF RIVERSIDE AND THE CALIFORNIA DEPARTMENT OF INDUSTRIAL RELATIONS (DIR) PRIOR TO THE START OF WORK. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE CITY OF RIVERSIDE AND THE CALIFORNIA DEPARTMENT OF INDUSTRIAL RELATIONS (DIR) PRIOR TO THE START OF WORK.



PLANNING DOCUMENTS

A3.0



OWNER:
 KATHY ADAMA, TRUSTEE OF THE CAPITAL FINANCIAL GROUP, INC. DEBTED BENEFIT PERSON TRUST
 KATHY A. ADAMA, TRUSTEE OF THE PARKS INTERNATIONAL, INC. DEBTED BENEFIT PERSON TRUST
 KATHY ADAMA, TRUSTEE OF THE 1011 INC. DEBTED BENEFIT PERSON TRUST

LESSEE: QUICK QUACK CAR WASH
 1300 LEAD HILL BLVD #200
 ROSSELLE CA, 92091

SITE ADDRESS:
 308 EAST ALESSANDRO BOULEVARD
 RIVERSIDE, CA 92506

ASSESSOR'S PARCEL NUMBER:
 712-000-004-7

TITLE INFORMATION:
 TITLE INFORMATION FOR THIS SURVEY BASED ON A PRELIMINARY REPORT PREPARED BY FACILITY MAINTENANCE
 DISTRICT AS ORDER NO. 191-200870-0-1001, AMENDMENT NO. 0 DATED: AUGUST 20, 2014

LEGAL DESCRIPTION:
 THE LAND REFERRED TO HEREIN BELOW IS SITUATED IN THE CITY OF RIVERSIDE IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS:
 THE EAST 200.71 FEET OF THE SOUTH 24.71 FEET OF THE EAST HALF OF THE SOUTH HALF OF THE SOUTH HALF OF THE WEST HALF OF LOT 4, BLOCK 14 OF ALESSANDRO TRACT, IN THE CITY OF RIVERSIDE, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS THE SAME ARE SHOWN IN BOOK 6, PAGE 15 OF MAPS IN THE OFFICE OF THE COUNTY RECORDER OF SAN BERNARDINO COUNTY, CALIFORNIA.

EXCEPT THAT PARCEL 1 WAS SOUTHERLY OF A LINE 40 FEET NORTH OF AND PARALLEL WITH THE SOUTH LINE OF SECTION 2, TOWNSHIP 3 SOUTH, RANGE 1 WEST, SAN BERNARDINO MERIDIAN, AS DESCRIBED BY ORDER TO THE COUNTY OF RIVERSIDE RECORDED JUNE 14, 1938 IN BOOK 1246, PAGE 308 OF OFFICIAL RECORDS.

VERTICAL BENCHMARK:
 DESCRIPTION: CITY OF RIVERSIDE BENCHMARK POINT ID "70-01" BEING A P.C. MARK AND CITY ENGINEER TAG IN SOUTHWEST CORNER OF THE WESTERN WALL OF A GARAGE BAY, 1' NORTHERLY OF THE FACE OF CURB ALONG THE NORTHERLY CURB OF ALESSANDRO BLVD, EASTERLY OF MESSON DRIVE PARKWAY.
 ELEVATION: 104.000' (NAD83)

SOURCE OF TOPOGRAPHY:
 FIELD SURVEY COMPLETED BY OMEGA LAND SURVEYING, INC. ON MAY 13, 2014.

BASIS OF BEARINGS:
 THE BASIS OF BEARINGS FOR THIS SURVEY IS THE CENTERLINE OF MESSON DRIVE PARKWAY NORTH AS SHOWN ON HD 10/19-12, SAG BEARING BEING 7° 00' 17.88" E

AREA SUMMARY:

REAL PARCEL AREA (NET)	36,484 SF
TOTAL PARCEL AREA (GROSS)	41,453 SF
X INTERIORS (PROPOSED)	768
X INTERIORS (EXISTING)	206

EASEMENTS:
 ① 10'-0" OF -WAY EASEMENT GRANTED TO THE CITY OF RIVERSIDE FOR PUBLIC STREET AND HIGHWAY PURPOSES RECORDED MAY 5, 1941, AS DOCUMENT NO. 130302



SEAN M. SAVAGE P.C.E. 76777 DATE

EXISTING LEGEND

ITEM	SYMBOL
PROPERTY LINE	---
CONVERSE	---
RIGHT-OF-WAY	---
EX. RETAINING WALL	---
EX. CURB & GUTTER	---

PROPOSED LEGEND

ITEM	SYMBOL
PROPOSED CONVERSE	---
PROPOSED GRADE BREAK	---
PROPOSED FINISH FLOOR ELEVATION	FF-578.00
PROPOSED TOP OF CURB ELEVATION	574.00E
PROPOSED FINISHMENT ELEVATION	574.00E
PROPOSED TOP OF WALL ELEVATION	574.00E
PROPOSED GRADE AT BASE OF WALL ELEV.	574.00E
PROPOSED FLOORLINE ELEVATION	574.00E
PROPOSED FINISH GRADE ELEVATION	574.00E
PROPOSED GRADIENT	1:5E
PROPOSED 6" PCC CURB	---
PROPOSED 6" PCC CURB & GUTTER	---
PROPOSED FLOW LINE	---
PROPOSED BUILDING	---
PROPOSED WALL	---
PROPOSED PARKING STALL STOPPING	---
PROPOSED HANDICAP STOPPING	---
PROPOSED P.V. STORM DRAIN (SIZE PER PLAN)	---
PROPOSED BRAMP STABILIZATION	---
PROPOSED P.V. STORM DRAIN INLET/ALLOW OUT/CONNECTION (SIZE AND TYPE PER PLAN)	---
PROPOSED P.V. INVERT	---
PROPOSED SHALE	---
PROPOSED P.V. SEWER LINE (SIZE PER PLAN)	---
PROPOSED P.V. PCC PAVEMENT MARK	---
PROPOSED P.V. PCC PAVEMENT PARKING	---
PROPOSED P.V. AC PAVEMENT PARKING	---
PROPOSED P.V. AC PAVEMENT HEAVY DRIVE WISE	---
PROPOSED P.V. LANDSCAPING	---
PROPOSED P.V. SIGNIFICATION	---
PROPOSED DRIVEWAY	---
PROPOSED OFFICE CURB OUTLET	---
PROPOSED OFFICE PCC PAVEMENT	---

SHEET INDEX:

SHEET	DESCRIPTION
T1.0	TITLE SHEET
A1.0	ARCHITECTURAL SITE PLAN
A1.1	LANDSCAPE PLAN
F1.0	PHOTOMETRIC SITE PLAN
A1.0	FLOOR PLAN
A1.1	ELEVATION FLOOR PLAN
A1.2	ROOF PLAN
A1.0	EXTERIOR ELEVATIONS
A1.1	INTERIOR ELEVATIONS
A1.2	FINISH WALL FINISH ELEVATIONS
A1.3	FINISH ELEVATIONS
A1.4	COLORS BUILDING ELEVATIONS
A1.5	COLORS BUILDING ELEVATIONS
A1.6	PAV CHANTRY
A1.1	PAV CHANTRY COLOR ELEVATIONS
A1.2	EAST WAGON CHANTRY
A1.3	WEST WAGON CHANTRY
A1.4	WAGON CHANTRY CURB ELEVATIONS
A1.0	SITE ELEMENTS
A1.1	SITE ELEMENTS
C1.0	SITE CONFORMANCE
C1.0	CONCEPTUAL DRAINAGE PLAN
C1.0	SITE DTM MAP



REVISIONS

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QUICK QUACK CAR WASH STORE #4-2022
 308 ALESSANDRO BLVD
 RIVERSIDE, CA 92506

TITLE SHEET

DATE: JANUARY 2020
 QUA PROJECT #: 20091

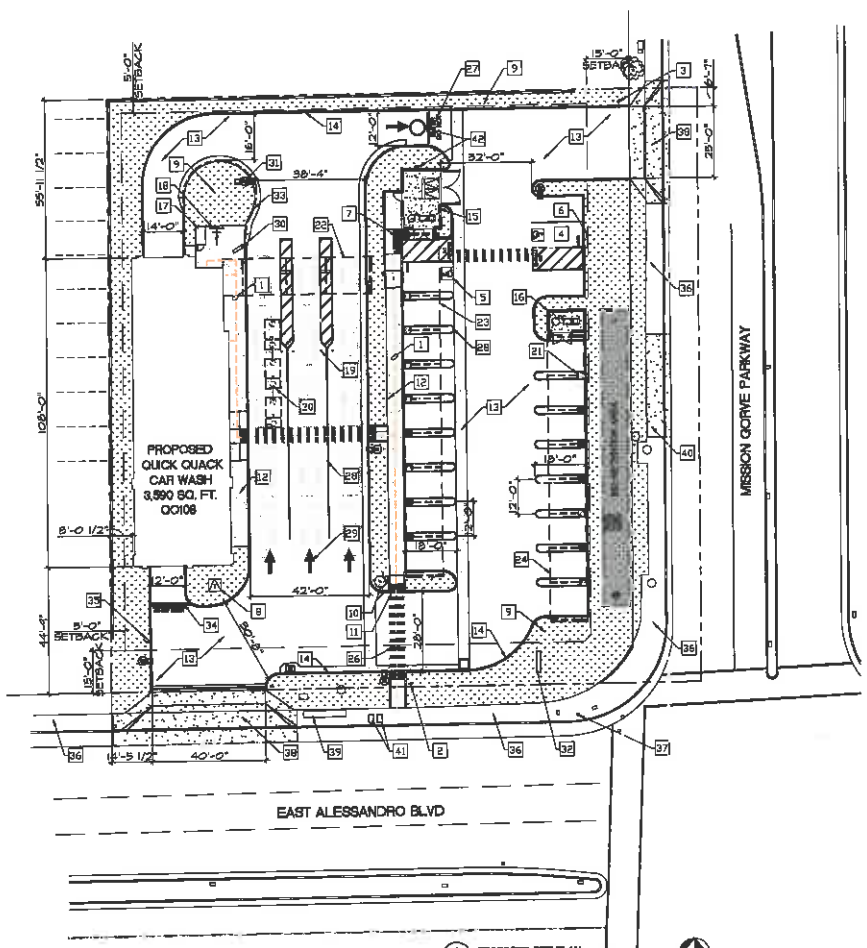
THIS DOCUMENT IS THE PROPERTY OF OMEGA ENGINEERING CONSULTANTS. IT IS TO BE USED ONLY FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREON. IT IS NOT TO BE REPRODUCED, COPIED, OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF OMEGA ENGINEERING CONSULTANTS.



PLANNING DOCUMENTS

T1.0

PREPARED BY: SEAN M. SAVAGE, LICENSED PROFESSIONAL ENGINEER, STATE OF CALIFORNIA, LICENSE NO. 76777
 DATE: 01/13/2020 10:52:00 AM



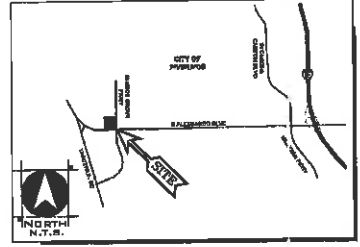
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- 25 NEW ARTISAN "WOODCRETE" PREGAST WALL - ENTIRE LENGTH OF PROPERTY - LAST 8'-0" SHALL BE 4'-0" TALL
- 26 NEW HIGHWAY PAINT WHITE 12" SIDE PEDESTRIAN STRIPING AT 36" O.C. (TYPICAL 3 PLACES)
- 27 NEW HIGHWAY PAINT YELLOW 18" "DO NOT ENTER" (TYPICAL 2 PLACES)
- 28 NEW HIGHWAY PAINT YELLOW 4" STRIPING
- 29 NEW HIGHWAY PAINT YELLOW PAVEMENT MARKINGS (TYPICAL)
- 30 NEW DISPLAY/PRICE SIGN - REFERENCE ONLY UNDER SEPARATE PERMIT (TYPICAL 3 PLACES)
- 31 NEW LED SITE LIGHTS (TYPICAL 5 PLACES) SEE PHOTOMETRIC PLAN
- 32 NEW MONUMENT SIGN - REFERENCE ONLY UNDER SEPARATE PERMIT
- 33 24" ROLLED CONCRETE CURB
- 34 NEW TRENCH DRAIN
- 35 NEW "STOP/GO" SIGN
- 36 EXISTING PUBLIC CONCRETE CURBS, AND GUTTER TO REMAIN (REPAIR AS REQUIRED BY PUBLIC WORKS)
- 37 EXISTING STOP LIGHT TO REMAIN
- 38 NEW DRIVE APPROACH - SEE CIVIL DRAWINGS
- 39 EXISTING BENCH STOP TO REMAIN
- 40 NEW CONCRETE SIDEWALK - SEE CIVIL DRAWINGS
- 41 NEW WATER METER - SEE CIVIL DRAWINGS
- 42 RIMP DUMP AND CONTROL LED - SEE CIVIL DRAWINGS

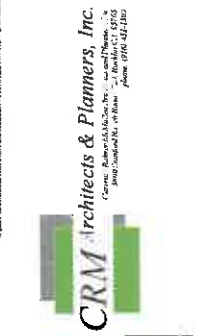
PARKING ANALYSIS

NEW 8'-0" X 14'-0" STANDARD STALL	2 STALLS
NEW 12'-0" X 18'-0" ADA STALL	1 STALL
NEW 12'-0" X 18'-0" VACUUM STALLS	17 STALLS
TOTAL STALLS	20 STALLS
NEW BIKE PARKING SPACES	3 SPACES
NEW LONG TERM BIKE PARKING SPACES	13 SPACES

VICINITY MAP



SITE NUMBER 4-802
BUILDING 00-108
SITE AREA 43,258
STACKING LANES 3 LANES
STACKING WIDTH 52'-0"
VACUUM STALLS 17 - 12'-0" X 18'-0"
VACUUM PRODUCER 2 - 40HP
ADA PARKING - 1 - 8'-0" X 18'-0"
PARKING STALLS 2 - 8' X 18'-0"
BUILDING AREA 3,990 S.F.
BUILDING SPRINKLED NO
OR STATION ATTACHED YES
BUILDING LENGTH 128'-0"
SHOWROOM LENGTH 128'-8"
CONVEYOR TRENCH LENGTH 126'-8"
CONVEYOR LENGTH 125'-0"
CONVEYOR TYPE SEAR MODEL PULL
SIGNATURE _____
APPROVAL DATE _____



REVISIONS

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QUICK QUACK CAR WASH
STORE #4-202
300 ALESSANDRO BLVD
RIVERSIDE, CA 92508

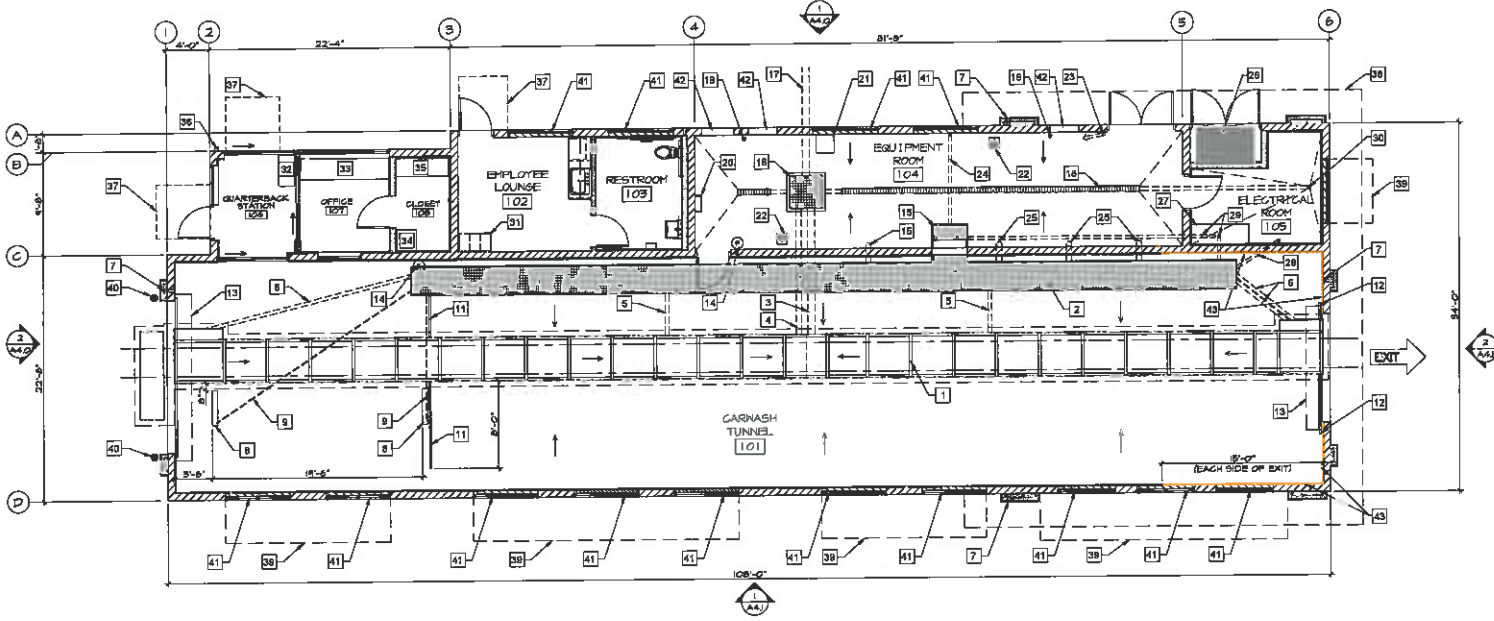
ARCHITECTURAL SITE PLAN

DATE JANUARY 2021
ONE PROJECT # 200

THE CONTRACTOR SHALL OBTAIN ALL NECESSARY APPROVALS FROM THE CITY OF RIVERSIDE AND THE CALIFORNIA DEPARTMENT OF INDUSTRIAL RELATIONS AND THE CALIFORNIA DEPARTMENT OF INDUSTRIAL RELATIONS AND THE CALIFORNIA DEPARTMENT OF INDUSTRIAL RELATIONS AND THE CALIFORNIA DEPARTMENT OF INDUSTRIAL RELATIONS.



PLANNING DOCUMENTS



1 PROPOSED CAR WASH FLOOR PLAN
SCALE: 3/16" = 1'-0"

KEYNOTES

- 1 CONVEYOR TRENCH SLOPE AT 1/4" PER FOOT TO DRAIN.
- 2 UTILITY TRENCH, SLOPE BOTTOM TO DRAIN.
- 3 6" PIPE FROM CONVEYOR TRENCH TO CLEAN OUT PIT.
- 4 6" OVERFLOW PIPE FROM CONVEYOR TRENCH TO CLEAN OUT PIT.
- 4' FLOOR DRAIN FROM UTILITY TRENCH TO THE CONVEYOR TRENCH SPACED EQUALLY IN UTILITY TRENCH (TYPICAL 2 PLACES)
- 4' PIPE CHASE FROM END OF UTILITY TRENCH TO CONVEYOR TRENCH (TYPICAL 3 PLACES)
- 7 HOSE BIB IN RECESS BOX, PROVIDE WITH LOCKABLE DOOR AT EXTERIOR LOCATIONS, SEE PLUMBING DRAWINGS (TYPICAL 4 PLACES)
- 8 3/4" DEEP X 6" WIDE X 3'-3" LONG RECESS IN SLAB FOR TIRE SENSOR SWITCH (TYPICAL 2 PLACES)
- 8 1/2" CONDUIT AT END OF RECESS TO UTILITY TRENCH FOR TIRE SWITCH, CONDUIT TO GO UNDER CONVEYOR TRENCH (TYPICAL 2 PLACES)
- 10 NOT USED
- 11 5/4" X 1 1/2" PIPE RECESS IN SLAB FOR DRAINAGE, SLOPE EACH SIDE TO CONVEYOR TRENCH
- 12 3/4" X 3 1/2" WIDE RECESS IN SLAB FOR DRAINAGE, SLOPE EACH SIDE TO CONVEYOR TRENCH, EXTENDING 1'-0" PAST EXIT OPENING
- 13 COLLING ROLL-UP DOOR, (TYPICAL 2 PLACES)
- 14 WALL MOUNTED ENCLOSED FIRE EXTINGUISHER - 2A-10BC (TYPICAL 2 PLACES)
- 15 3'-6" WIDE X 2'-2" DEEP OPEN FOR ACCESS TO UTILITY TRENCH, CONTRACTOR TO PROVIDE COVER - MATCH UTILITY TRENCH
- 16 3'-0" X 3'-0" RECLAIM CLEAN-OUT
- 17 8" PIPE FROM CLEAN OUT PIT TO GREASE INTERCEPTOR (CLARIFIED), TRENCH DRAIN
- 18 ROOF DRAIN LEADER (TYPICAL 3 PLACES)
- 19 ROOF ACCESS LADDER
- 20 CONTRACTOR SUPPLIED AND INSTALLED EYE WASH STATION
- 21 16" X 16" X 12" DEEP FLOOR SINK, PLACE SINK AS CLOSE TO WALL AS POSSIBLE WITHOUT INTERFERING WITH FOOTING (TYPICAL 2 PLACES)

- 23 1" PVC CHASE TO BUS JICE CONTROL PANEL
- 24 3" CHASE FROM ACCESS OPENING TO FACE OF WALL
- 25 6" SNEEPS FROM UTILITY TRENCH TO EQUIPMENT ROOM (TYPICAL 4 PLACES)
- 26 ELECTRICAL SERVICE SWITCHGEAR
- 27 2'-0" X 3'-6" X 4" TALL CONCRETE 'HOUSE CLEANING PAD' FOR MCG PANEL
- 28 3" CHASE (NEXT TO CONTROL PANEL TO UTILITY TRENCH)
- 29 2 - 8" CHASE AT CENTER OF MCG PANEL, 'HOUSE KEEPING PAD' RUN ONE CHASE TO EQUIPMENT TRENCH AND ONE NEXT TO ACCESS OPENING
- 30 4" FLOOR DRAIN - CONNECT TO TRENCH DRAIN
- 31 EMPLOYEE LOCKERS, (TOTAL OF 3 - 1 ADA ACCESSIBLE) CONTRACTOR SUPPLIED AND INSTALLED
- 32 CONTRACTOR FURNISHED AND INSTALLED 18" X 36" X 40" TALL POS CABINET
- 33 34" MAXIMUM A.F.P. COUNTER TOP
- 34 WALL MOUNTED IT CABINET
- 35 CONTRACTOR FURNISHED AND INSTALLED 30" X 34" X 78" TALL STORAGE CABINET WITH ADJUSTABLE SHELVING AND WHITE MELAMINE FINISH ON THE INSIDE AND GRAY OUTSIDE
- 36 KNOX BOX - CONTRACTOR TO COORDINATE WITH LOCAL FIRE DEPARTMENT FOR TYPE, SIZE AND PREFERRED LOCATION
- 37 LEVEL LANDING PER CBC 103-104.2.4 AND TABLE 103-104.2.4 (TYP) - 24" MIN STRIKE EDGE CLEARANCE FOR THE EXTERIOR DOOR LANDING
- 38 LINE OF ROOF ABOVE
- 39 WALL MOUNTED CANOPY ABOVE, (TYPICAL 3 PLACES)
- 40 4" CONCRETE FILLED PIPE BOLLARD AT ENTRANCE (TYPICAL 2 PLACES)
- 41 FAUX HINDON - SEE EXTERIOR ELEVATIONS
- 42 32" WIDE X 16" HIGH AIR VENT - SEE EXTERIOR ELEVATIONS (TYP. 3 PLACES)
- 43 AGGUSTIBLOK 'GRAY FIBER' PANELS

NOTE:
ALL CONDUITS TO HAVE SNEEPS ONLY, 45 DEGREE CONDUIT/CHASE BENDS ARE NOT ALLOWED

CRM Architects & Planners, Inc.
10000 Redwood Valley Blvd., Redwood Valley, CA 95470
707.938.1100
www.crmarchitects.com



Quick Quack
CAR WASH

REVISIONS

#	DATE	DESCRIPTION

QUICK QUACK CAR WASH
STORE #4-202
308 ALBERNDRO BLVD
RIVERSIDE, CA 92508

FLOOR PLAN

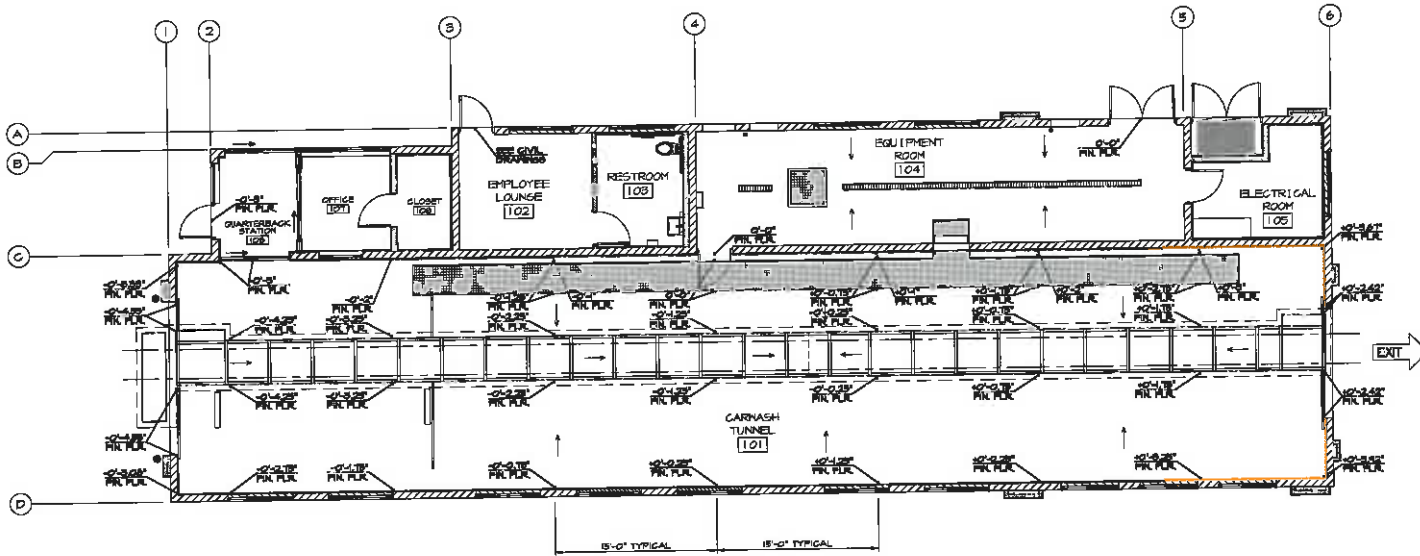
DWG: JANUARY 2021
CRM PROJECT # 2021

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PLANNING DOCUMENTS

A3.0



1 CAR WASH FINISH FLOOR ELEVATIONS
SCALE: 3/8" = 1'-0"

CRM Architects & Planners, Inc.
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 www.crmarchitects.com



Quick Quack
CAR WASH

REVISIONS

#	DATE	DESCRIPTION

QUICK QUACK CAR WASH
STORE #4-202
306 ALESSANDRO BLVD
RIVERSIDE, CA 92508

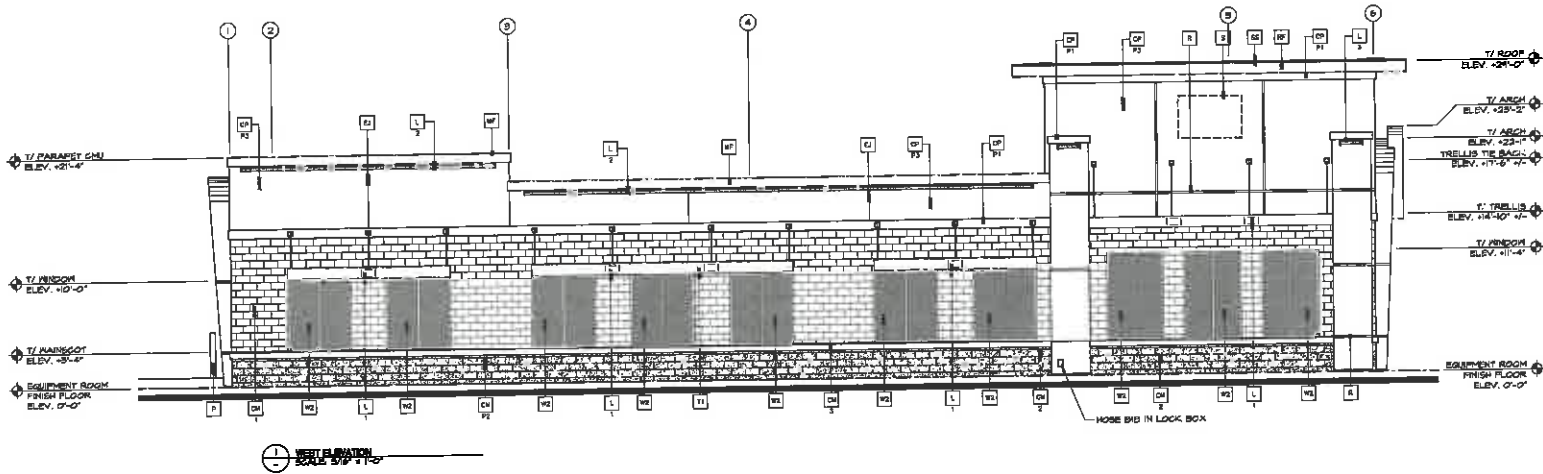
ELEVATION FLOOR PLAN

DATE: JANUARY 2021
 ORCA PROJECT #: 2285

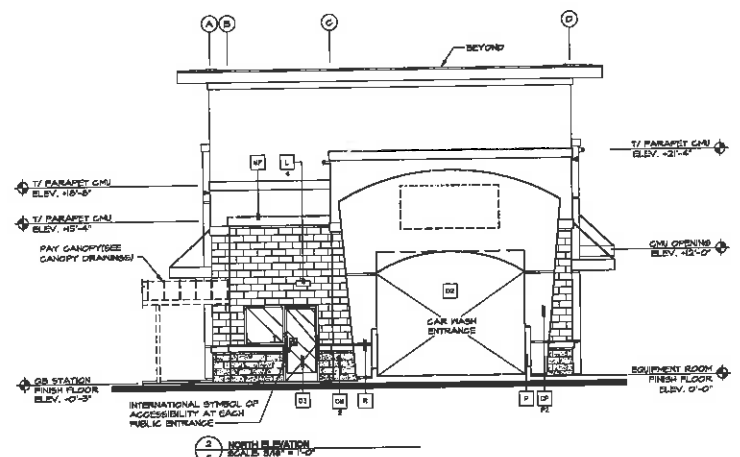
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PLANNING DOCUMENTS



1 WEST ELEVATION
SCALE 3/8" = 1'-0"



2 NORTH ELEVATION
SCALE 3/8" = 1'-0"

KEY NOTES:

- 01 ANSELUS BLOCK BURNISHED SMOOTH FACE CONCRETE MASONRY UNITS - MEDIUM HEIGHT NATURAL GRAY WITH INTEGRAL FACTORY "RAINBLOCC" WATER REPELLENT ADHESURE. MORTAR TO HAVE "RAINBLOCC" FOR MORTAR ADHESURE. MORTAR COLOR TO MATCH.
- 02 ANSELUS BLOCK SPLIT FACE CONCRETE MASONRY UNITS - MEDIUM HEIGHT GREYSTONE WITH INTEGRAL FACTORY "RAINBLOCC" WATER REPELLENT ADHESURE. MORTAR TO HAVE "RAINBLOCC" FOR MORTAR ADHESURE. MORTAR COLOR TO MATCH.
- 03 ANSELUS BLOCK BURNISHED SMOOTH FACE CONCRETE MASONRY UNIT SILL - MEDIUM HEIGHT GREYSTONE WITH INTEGRAL FACTORY "RAINBLOCC" WATER REPELLENT ADHESURE. MORTAR TO HAVE "RAINBLOCC" FOR MORTAR ADHESURE. MORTAR COLOR TO MATCH.

04 ACRYLIC PLASTER COLOR COAT
COLOR DESIGNATION

- P1 = MATCH SHERWIN WILLIAMS "QUICK QUACK GREEN"
- P2 = MATCH SHERWIN WILLIAMS #1002 "DECISIVE YELLOW"
- P3 = MATCH SHERWIN WILLIAMS #1004 "SNOWBOUND"
- PLASTER COLOR COAT TO BE SAND FINISH
- 05 HOLLOW METAL DOOR - PAINT TO MATCH SHERWIN WILLIAMS #1023 "BACKDROP"
- 06 OVERHEAD METAL DOOR - PAINT TO MATCH SHERWIN WILLIAMS #1023 "BACKDROP"
- 07 CLEAR ANODIZED ALUMINUM COMMERCIAL GRADE STOREFRONT GLASS DOOR

08 8" X 16" THROUGH WALL SCUPPER COLLECTOR AND DOWNSPOUT. PAINT TO MATCH ADJACENT SURFACE

- 09 1/4" DEEP REVEAL EXPANSION JOINT
- L LIGHT FIXTURE
TYPE:
L1 = LED RADIAL WALL SCOURGE (DOWN ONLY)
L2 = LINEAR FACADE WALL LIGHT (DOWN ONLY)
L3 = 2 FOOT LED PLASTER LIGHT (DOWN ONLY)
L4 = WALL PACK (DOWN ONLY)
CONTACT STEVE FRIEDMAN (847) 850-1444 WITH HERMITAGE LIGHTING FOR ORDERING LIGHTING. (NO EXCEPTIONS)

- M1 METAL FASCIA - PAINT TO MATCH SHERWIN WILLIAMS "QUICK QUACK GREEN" (GREEN)
- N1 4" PIPE BOLLARD FILLED WITH CONCRETE WITH PROTECTIVE BELIEVE
- N2 2" WIDE X 1/4" DEEP CLEAR ANODIZED ALUMINUM PLASTER REVEAL
- M2 METAL ROOF FASCIA - MATCH SHERWIN WILLIAMS "QUICK QUACK GREEN" (GREEN)
- M3 3" DIA PVC ROOF OVERLAP SCUPPER - PAINT TO MATCH ADJACENT FINISH
- E SIGNAGE BY OTHERS SHOWN DASHED. FOR REFERENCES ONLY UNDER SEPARATE PERMIT. GENERAL CONTRACTOR TO COORDINATE WITH SIGN COMPANY.

- 08 STANDING BEAM METAL ROOF FIRESTONE RUB - "DARK IVY"
- 11 METAL WALL GANTRY SYSTEM - COLOR TO MATCH SHERWIN WILLIAMS "QUICK QUACK GREEN"
- 12 WALL GANTRY TIE-BACK SYSTEM - COLOR TO MATCH SHERWIN WILLIAMS "QUICK QUACK GREEN"
- V 3/2" WIDE X 1/2" HIGH VENT - PAINT TO MATCH ADJACENT FINISH
- M1 EXTERIOR WINDOW ASSEMBLY - TEMPERED GLASS CLEAR ANODIZED ALUMINUM STOREFRONT
- 10 EXTERIOR PAINT WINDOW ASSEMBLY - WITH BLACK GRANITE FELT TEMPERED SPANDREL GLASS CLEAR ANODIZED ALUMINUM STOREFRONT

REVISIONS & DATE DESCRIPTION

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**QUICK QUACK CAR WASH
STORE #4-202
306 ALESSANDRO BLVD
RIVERSIDE, CA 92508**

EXTERIOR ELEVATIONS

DATE: JANUARY 2025
CRM PROJECT #: 2025



PLANNING DOCUMENTS

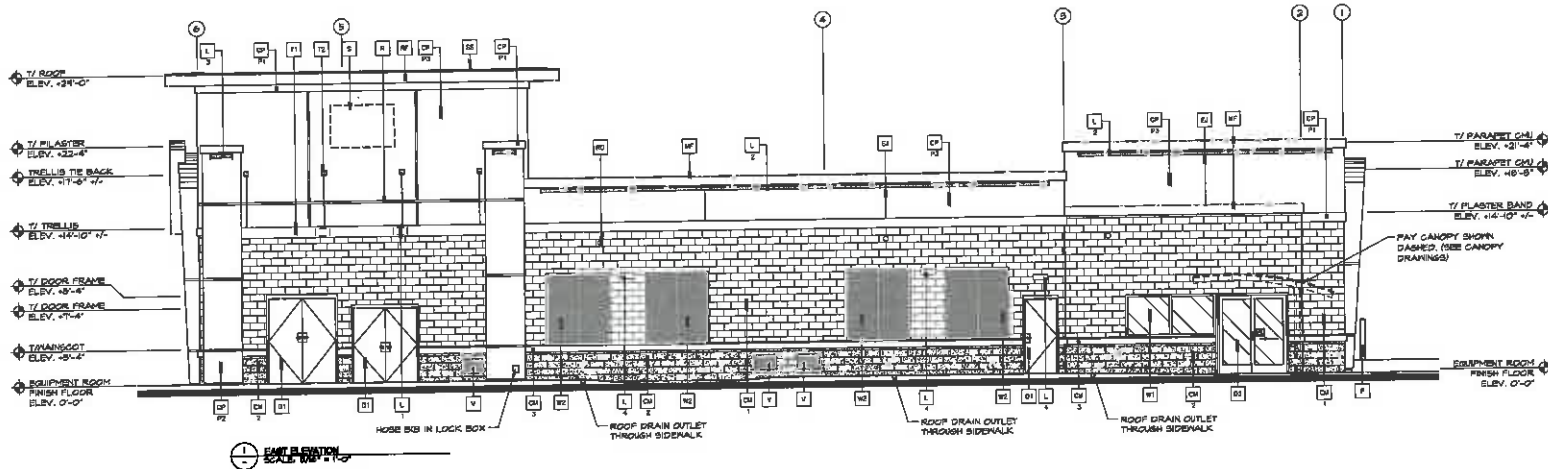
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1000 N. 1st Street, Suite 100
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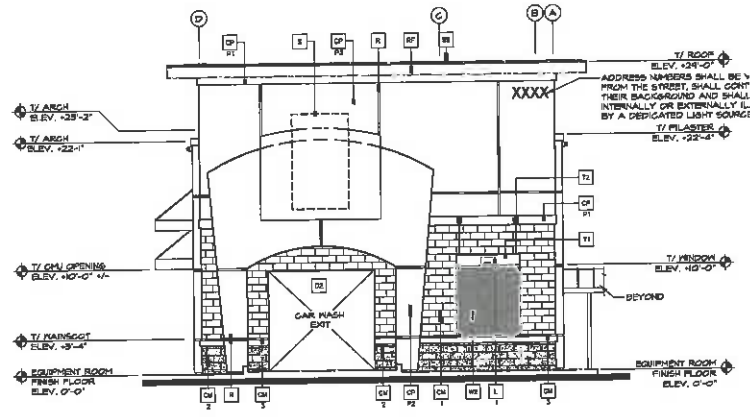


**Quick Quack
CAR WASH**

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1 NORTH ELEVATION
SCALE: 1/8" = 1'-0"



2 SOUTH ELEVATION
SCALE: 1/8" = 1'-0"

KEY NOTES:

- 01 ANGELUS BLOCK BURNISHED SMOOTH FACE CONCRETE MASONRY UNITS - MEDIUM HEIGHT NATURAL GRAY WITH INTEGRAL FACTORY "RAINBLOC" WATER REPELLENT ADMIXTURE. MORTAR TO HAVE "RAINBLOC" FOR MORTAR ADMIXTURE. MORTAR COLOR TO MATCH.
- 02 ANGELUS BLOCK SPLIT FACE CONCRETE MASONRY UNITS - MEDIUM HEIGHT GREYSTONE WITH INTEGRAL FACTORY "RAINBLOC" WATER REPELLENT ADMIXTURE. MORTAR TO HAVE "RAINBLOC" FOR MORTAR ADMIXTURE. MORTAR COLOR TO MATCH.
- 03 ANGELUS BLOCK BURNISHED SMOOTH FACE CONCRETE MASONRY UNIT SILL - MEDIUM HEIGHT GREYSTONE WITH INTEGRAL FACTORY "RAINBLOC" WATER REPELLENT ADMIXTURE. MORTAR TO HAVE "RAINBLOC" FOR MORTAR ADMIXTURE. MORTAR COLOR TO MATCH.

- 04 ACRYLIC PLASTER COLOR COAT
- 05 COLOR DESIGNATION:
 - F1 = MATCH SHERWIN WILLIAMS "QUICK QUACK GREEN"
 - F2 = MATCH SHERWIN WILLIAMS #6102 "DEBRIVE YELLOW"
 - F3 = MATCH SHERWIN WILLIAMS #1004 "SANDSTONE"
- 06 PLASTER COLOR COAT TO BE SAND FINISH
- 07 HOLLOW METAL DOOR - PAINT TO MATCH SHERWIN WILLIAMS #1025 "BACKDROP"
- 08 OVERHEAD METAL DOOR - PAINT TO MATCH SHERWIN WILLIAMS #1025 "BACKDROP"
- 09 CLEAR ANODIZED ALUMINUM COMMERCIAL GRADE STOREFRONT GLASS DOOR

- 10 8" X 16" THROUGH WALL SCUPPER, COLLECTOR AND DOWNSPOUT. PAINT TO MATCH ADJACENT SURFACE
- 11 1/4" DEEP REVEAL EXPANSION JOINT
- 12 LIGHT FIXTURE
- 13 TYPE:
 - L1 = LED RADIAL WALL SCORGE (DOWN ONLY)
 - L2 = LINEAR FACIAS WALL LIGHT (DOWN ONLY)
 - L3 = 2 FOOT LED PILASTER LIGHT (DOWN ONLY)
 - L4 = WALL PACK (DOWN ONLY)
- 14 CONTACT STEVE FRIEDMAN | 841.880.4444 WITH HERITAGE LIGHTING FOR ORDERING LIGHTING. (NO EXCEPTIONS)

- 15 METAL FASCIA - PAINT TO MATCH SHERWIN WILLIAMS "QUICK QUACK GREEN"
- 16 4" PIPE BOLLARD FILLED WITH CONCRETE WITH PROTECTIVE SLEEVE
- 17 3" WIDE X 1/4" DEEP CLEAR ANODIZED ALUMINUM FLASHER REVEAL
- 18 METAL ROOF FASCIA - MATCH SHERWIN WILLIAMS "QUICK QUACK GREEN" (GREEN)
- 19 3" DIA PVC ROOF OVERFLOW SCUPPER - PAINT TO MATCH ADJACENT FINISH
- 20 SIGNAGE BY OTHERS SHOWN DASHED - FOR REFERENCE ONLY UNDER SEPARATE PERMIT. GENERAL CONTRACTOR TO COORDINATE WITH SIGN COMPANY.

- 21 STANDING SEAM METAL ROOF
- 22 FIRESTONE UCS - "DARK IVY"
- 23 METAL WALL CANOPY SYSTEM - COLOR TO MATCH SHERWIN WILLIAMS "QUICK QUACK GREEN"
- 24 WALL CANOPY TIE-BACK SYSTEM - COLOR TO MATCH SHERWIN WILLIAMS "QUICK QUACK GREEN"
- 25 32" WIDE X 16" HIGH VENT - PAINT TO MATCH ADJACENT FINISH
- 26 EXTERIOR WINDOW ASSEMBLY - TEMPERED GLASS CLEAR ANODIZED ALUMINUM STOREFRONT
- 27 EXTERIOR FAUX PENDON ASSEMBLY - WITH BLACK CERAMIC FRUIT TEMPERED SPANDREL GLASS CLEAR ANODIZED ALUMINUM STOREFRONT

REVISIONS & DATE DESCRIPTION

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QUICK QUACK CAR WASH STORE #4-202
 308 ALBERNDORO BLVD
 FRESNO, CA 93708

BUILDING ELEVATIONS

DATE: JANUARY 2020
 CRM PROJECT #: 0020

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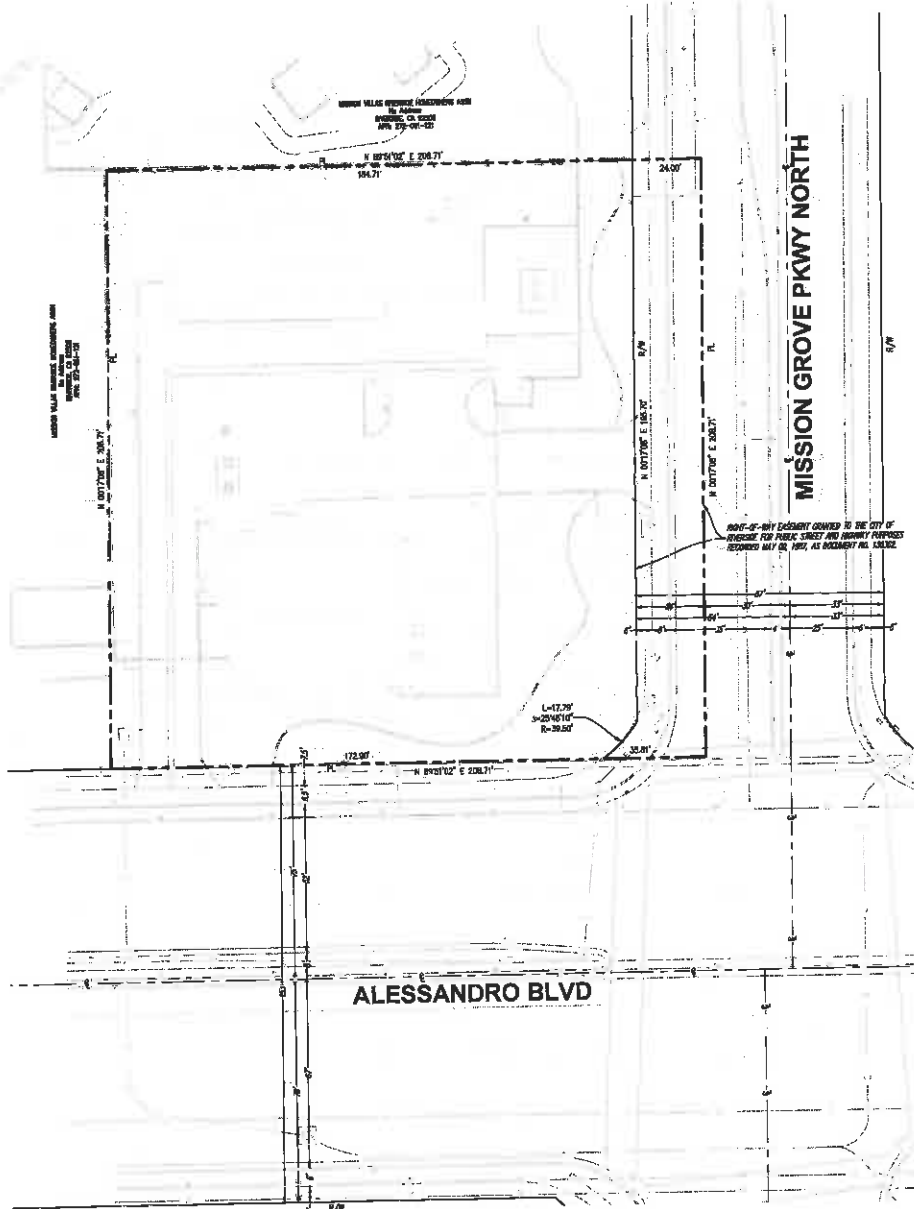
PLANNING DOCUMENTS

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 5500 N. Zedeno - 1, Suite 100, Fresno, CA 93708
 Phone: (559) 431-1200



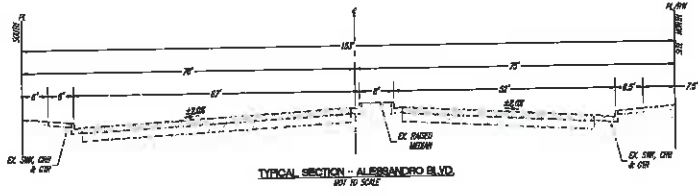
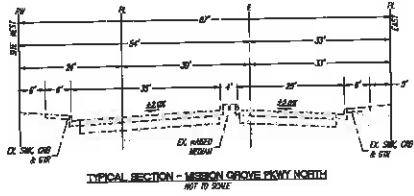
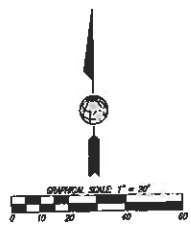
Quick Quack CAR WASH

REVISION: P. JAMES GIBBS (FORM ALESSANDRO BLVD) - 02/27/2007; P. JAMES GIBBS (FORM ALESSANDRO BLVD) - 02/27/2007; P. JAMES GIBBS (FORM ALESSANDRO BLVD) - 02/27/2007; P. JAMES GIBBS (FORM ALESSANDRO BLVD) - 02/27/2007

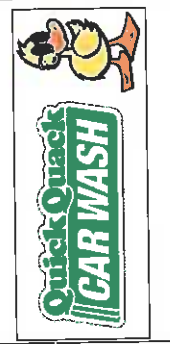


EXISTING LEGEND

- DEM
- PROPERTY LINE
- CENTERLINE
- RIGHT-OF-WAY
- EX. REMAINING WALL
- EX. CURB & GUTTER



OMEGA
ENGINEERING CONSULTANTS
 4340 VIEWBRIDGE AVE. SUITE B
 SAN DIEGO, CA 92123
 PH: (658) 634-8620 FAX: (658) 634-8627



REVISIONS

DATE	DESCRIPTION

QUICK QUACK CAR WASH
STORE 84-202
308 ALESSANDRO BLVD
RIVERSIDE, CA 92508

CONSTRAINTS
MAP

DATE: JANUARY 2008
CRM PROJECT #: 2580

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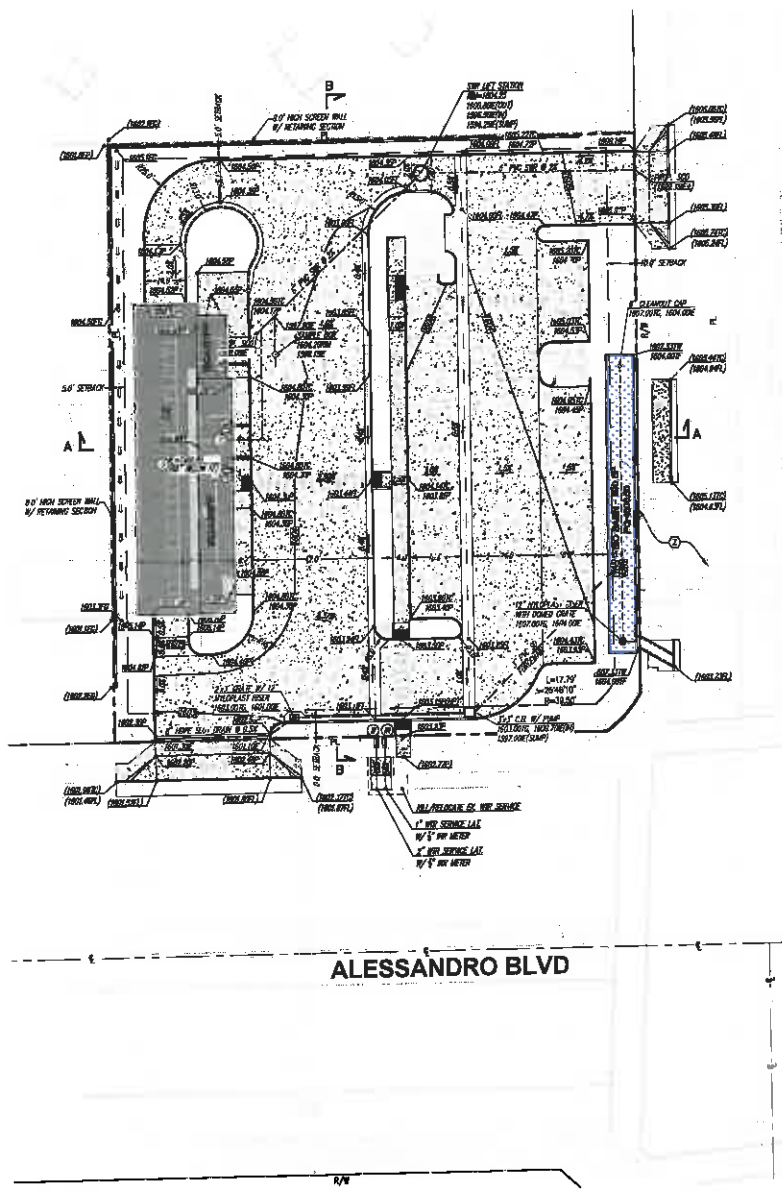
PLANNING DOCUMENTS

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SEAN H. SHAWAR R.C.E. 70877 DATE

DRAWING: 1. PRELIMINARY GRADING PLAN (DATE: 11/30/2010) SHEET: 01 OF 01



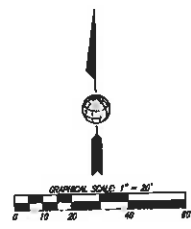
MISSION GROVE PKWY NORTH

GRADING INFORMATION

CUT (TO FINISH SURFACE)	522	CY
FILL (TO FINISH SURFACE)	434	CY
DEM & LANDSCAPING	812	CY
UNDERCUTS	528	CY
EXPORT (BALANCE)	483	CY

EASEMENTS

1. WIDTH-OF-WAY EASEMENT GRANTED TO THE CITY OF RIVERSIDE FOR PUBLIC STREET AND HIGHWAY PURPOSES RECORDED MAY 8, 1981, AS SECURITY NO. 150162.

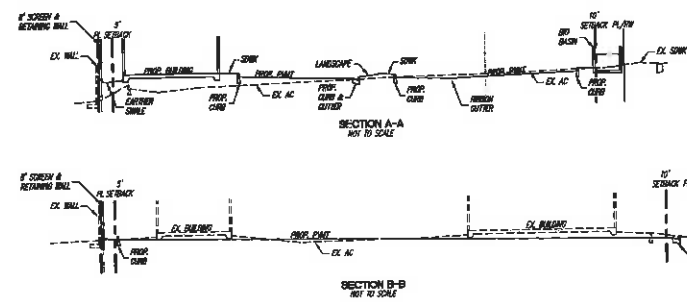


EXISTING LEGEND

DEM	DEM
PROPERTY LINE	---
CENTERLINE	---
POINT-OF-BUY	---
EX. RETAINING WALL	---
EX. CURB & GUTTER	---

PROPOSED LEGEND

DEM	DEM
PROPOSED CONTOUR	---
PROPOSED GRADE BREAK	---
PROPOSED FINISH FLOOR ELEVATION	FF-078.00
PROPOSED TOP OF CURB ELEVATION	274.00E
PROPOSED FINISHMENT ELEVATION	324.00E
PROPOSED TOP OF WALL ELEVATION	324.00E
PROPOSED GRADE AT BASE OF WALL ELEV.	324.00E
PROPOSED FINISH FLOOR ELEVATION	324.00E
PROPOSED FINISH GRADE ELEVATION	324.00E
PROPOSED CONDUIT	---
PROPOSED 6\"/>	



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OMEGA

ENGINEERING CONSULTANTS
 4340 VIEWBRIDGE AVE. SUITE B
 SAN DIEGO, CA 92123
 PH: (619) 634-8620 FAX: (619) 634-8627



REVISIONS

NO.	DATE	DESCRIPTION
1		
2		
3		
4		
5		
6		
7		
8		

QUICK QUACK CAR WASH
 STORE #4-292
 308 ALESSANDRO BLVD
 RIVERSIDE, CA 92508

PRELIMINARY GRADING PLAN

DATE: JANUARY 2010
 CDM PROJECT #: 808

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PLANNING DOCUMENTS

C2.0

NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893**. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The City of Riverside should be contacted on non-ALUC issues. For more information please contact City of Riverside Planner Ms. Candice Assadzadeh at (951) 826-5667.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to prull@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please telephone Barbara Santos at (951) 955-5132.

PLACE OF HEARING: Riverside County Administration Center
4080 Lemon Street, 1st Floor Board Chambers
Riverside California

DATE OF HEARING: May 13, 2021

TIME OF HEARING: 9:30 A.M.

Pursuant to Executive Order N-25-20, this meeting will be conducted by teleconference and at the Place of Hearing, as listed above. Public access to the meeting location will be allowed, but limited to comply with the Executive Order. Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org

CASE DESCRIPTION:

ZAP1457MA21 – Eugene Marini (Representative: Omega Engineering Consultants) – City of Riverside Case No. PR-2021-001023 (General Plan Amendment, Rezone, Conditional Use Permit, Design Review), a proposal to construct a 3,590 square foot drive-thru carwash building on 0.89 acres, located on the northwest corner of Mission Grove Parkway and Alessandro Boulevard. The applicant also proposes amending the site's General Plan land use designation from Office (O) to Commercial (C), and changing the site's zoning from Office (O) to Commercial General (CG). (Airport Compatibility Zone C2 of the March Air Reserve Base/Inland Port Airport Influence Area).



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP1457 MAR1 DATE SUBMITTED: 3/4/2021

APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

Applicant Omega Engineering Consultants Phone Number (619) 488-6293
Mailing Address 4340 Viewridge Ave Suite B, San Diego, CA 92123 Email max@omega-consultants.com

Representative Eugene Marini Phone Number (858) 281-6091
Mailing Address 5820 Oberlin Drive, Suite 201, San Diego, CA 92121 Email Eugene@KAenterprises.net

Property Owner Eugene Marini Phone Number (858) 281-6091
Mailing Address 5820 Oberlin Drive, Suite 201, San Diego, CA 92121 Email Eugene@KAenterprises.net

March
C2

LOCAL JURISDICTION AGENCY

Local Agency Name City of Riverside Phone Number (951) 826-5667
Staff Contact Candice Assadzadeh Email CAssadzadeh@riversideca.gov
Mailing Address _____ Case Type _____

Local Agency Project No Planning Case PR-2021-001023 (GPA, RZ, CUP, DR, VR)
 General Plan / Specific Plan Amendment
 Zoning Ordinance Amendment
 Subdivision Parcel Map / Tentative Tract
 Use Permit
 Site Plan Review/Plot Plan
 Other

PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address 306 East Alessandro Boulevard, Riverside, CA 92508
Assessor's Parcel No. 272-060-004-7 Gross Parcel Size 43,459(Gross) 38,484(Net)
Subdivision Name Map of the Alessandro Tract Nearest Airport and distance from Airport March Air Reserve Base - 2.94 Miles
Lot Number Lot 4, Block 4, Book 6, Page 13 of Maps

PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use Office
(describe) _____

Proposed Land Use (describe)	Commercial Retail		
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units) _____		
For Other Land Uses (See Appendix C)	Hours of Operation	7AM-7PM	
	Number of People on Site	57	Maximum Number 176
	Method of Calculation	3 employees, 5 cars in carwash (at max capacity), 15 cars in queue, 16 cars (1 in each vacuum stall). At 1.5 people per car we get 57 people on site.	
Height Data	Site Elevation (above mean sea level)	1603	ft.
	Height of buildings or structures (from the ground)	29.0	ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
	If yes, describe	_____ _____ _____	

- A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:**
1. Completed ALUC Application Form
 1. ALUC fee payment
 1. Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
 1. Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
 1. CD with digital files of the plans (pdf)
 1. Vicinity Map (8.5x11)
 1. Detailed project description
 1. Local jurisdiction project transmittal
 3. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
 3. Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. **(Only required if the project is scheduled for a public hearing Commission meeting)**

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.4

HEARING DATE: May 13, 2021

CASE NUMBER: ZAP1458MA21 – MVRP PV (Representative: Sunworks Inc.)

APPROVING JURISDICTION: City of Moreno Valley

JURISDICTION CASE NO: PEN20-0198 (Plot Plan)

LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

Airport Influence Area: March Air Reserve Base

Land Use Policy: Zone C1

Noise Levels: 60 – 65 CNEL contour

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends that the Commission **CONTINUE** the matter to the June 10, 2021 meeting, pending completion of the Air Force review of the project.

PROJECT DESCRIPTION: A proposal to construct a 5,588 square foot rooftop solar panel system on an existing industrial building on 1.62 acres.

PROJECT LOCATION: The site is at 14114 Business Center Drive, northerly of Golden Crest Drive, westerly of Elsworth Street, and southerly of Alessandro Boulevard, in the City of Moreno Valley, approximately 5,675 feet northerly of the northerly end of Runway 14-32 at March Air Reserve Base.

BACKGROUND:

Non-Residential Land Use Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zone C1 which limits average intensity to 100 people per acre and 250 people per single acre. The proposed rooftop solar panels will not generate any occupancy.

March Air Reserve Base/United States Air Force Input: Given that the project site is located in Zone C1 northerly of the northerly runway at March Air Reserve Base, the March Air Reserve Base staff

was notified of the proposal to add rooftop solar panels, and sent a solar glare hazard analysis study for their review. As of the time this staff report was prepared, no comments have been received from the Air Force regarding this project.

Flight Hazard Issues: Structure height, electrical interference, and reflectivity/glare are among the issues that solar panels in the airport influence area must address. The project's 5,588 square foot photovoltaic (PV) panel structures would be located on the rooftop of the existing industrial building within Compatibility Zone C1.

Glint and Glare/Reflectivity

Based on the Federal Aviation Administration's Interim Policy for Review of Solar Energy System Projects on Federally Obligated Airports, no glare potential or low potential for temporary after-image ("green" level) are acceptable levels of glare on final approach (within 2 miles from end of runway) for solar facilities located on airport property. However, potential for temporary after-image" ("yellow" level) and potential for permanent eye damage ("red" level) are not acceptable levels of glare on final approach. No glare is permitted at air traffic control towers.

The project proposes 5,588 square feet of solar panels on the existing building rooftop with a fixed tilt of 10 degrees with no rotation, and an orientation of 182 degrees. The applicant has submitted a glare analysis utilizing the web-based Forge Solar, a copy of which is attached hereto. The analysis was based on a 2 mile straight in approach (as per FAA Interim Policy standards) to runways 14 and 32, and also based on the traffic patterns as identified by March Air Reserve Base staff (Runway 12/30 General Aviation, Runway 14/32 General Aviation, Runway 14/32 C-17/KC-135, Runway 14/32 Overhead). The analysis utilized a glide slope approach of 5.0 degrees. No glare would affect the Air Traffic Control Tower.

The analysis concluded that no glare would occur on the 2 mile approach to runways 14 and 32. However, some potential for glare was identified within the Air Force traffic pattern. Evaluation of the Air Force traffic patterns indicates that the panels would result in low potential for temporary after-image ("green" level glare). All times are in standard time.

Runway 14/32 C-17/KC-135 traffic pattern:

- Runway 32 Crosswind portion, totaling 168 minutes of "green" level glare, lasting up to 5 minutes a day, in February to March, and October to November from 6:00 a.m. to 7:00 a.m.
- Runway 32 Upwind portion, totaling 799 minutes of "green" level glare, lasting up to 5 minutes a day, between October to April from 6:30 a.m. to 7:30 a.m.

Runway 12/30 General Aviation traffic pattern:

- Runway 30 Downwind portion, totaling 1,331 minutes of "green" level glare, lasting up to 20 minutes a day, between May to August from 5:00 p.m. to 6:00 p.m.

The total of 2,298 minutes of “green” level glare represents less than 1 percent of total day light time.

Electrical and Communication Interference

The applicant has indicated that they do not plan to utilize equipment that would interfere with aircraft communications. The PV panels themselves present little risk of interfering with radar transmission due to their low profiles. In addition, solar panels do not emit electromagnetic waves over distances that could interfere with radar signal transmissions, and any electrical facilities that do carry concentrated current will be buried beneath the ground and away from any signal transmission. There are no radar transmission or receiving facilities within the site.

Prohibited and Discouraged Uses: Glare from solar panels could potentially constitute a hazard to flight. However, based on the solar glare hazard analysis provided, the glare experienced would result in a low potential for temporary after-image (“green” level) which has been determined by the Federal Aviation Administration (FAA) to be an acceptable level for solar facilities on airports. Therefore, the hazard potential is low. Staff has included conditions to remedy unanticipated situations.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site within the 60-65 CNEL range from aircraft noise. The proposed solar panels are a non-noise sensitive use, therefore no mitigation measures are necessary.

Part 77: The elevation of Runway 14-32 at its northerly terminus is 1,535 feet above mean sea level (1,535 feet AMSL). At a distance of approximately 5,675 feet from the runway to the site, Federal Aviation Administration (FAA) review would be required for any structures with top of roof elevation exceeding 1,593 feet AMSL. The site’s finished floor elevation is 1,561 feet AMSL and the existing building height is 30 feet. The establishment of the proposed rooftop solar panels would increase the maximum elevation by 1 foot, for a top point elevation of 1,592 feet AMSL. Therefore, review by the FAA Obstruction Evaluation Service is not required.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site.
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight or circling climb following takeoff or toward an aircraft engaged in a

straight or circling final approach toward a landing at an airport, other than a DoD or FAA-approved navigational signal light or visual approach slope indicator.

- (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport to the extent as to result in a potential for temporary after-image greater than the low (“green”) level.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children’s schools, day care centers, libraries, hospitals, skilled nursing and care facilities, congregate care facilities, places of assembly (including but not limited to places of worship and theaters)
 - (f) Highly noise-sensitive outdoor nonresidential uses. Examples of noise-sensitive outdoor nonresidential uses that are prohibited include, but are not limited to, major spectator-oriented sports stadiums, amphitheatres, concert halls and drive-in theaters.
 - (g) Other Hazards to Flight
3. The attached “Notice of Airport in Vicinity” shall be provided to all prospective purchasers and occupants of the property and be recorded as a deed notice.
 4. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
 5. The maximum height of the building, including all roof-mounted equipment including solar panels, if any, shall be limited to 31 feet, and the maximum top point elevation shall not exceed 1,593 feet above mean sea level unless a “Determination of No Hazard to Air Navigation” letter authorizing a higher top point elevation has been issued by the Federal Aviation Administration Obstruction Evaluation Service.

6. All solar arrays installed on the project site shall consist of smooth glass photovoltaic solar panels without anti-reflective coating, a fixed tilt of 10 degrees and orientation of 182 degrees. Solar panels shall be limited to a total of 5,588 square feet, and the locations and coordinates shall be as specified in the glare study. Any deviation from these specifications (other than reduction in square footage of panels), including change in orientation, shall require a new solar glare analysis to ensure that the amended project does not result in any glare impacting the air traffic control tower or creation of any “yellow” or “red” level glare in the flight paths, and shall require a new hearing by the Airport Land Use Commission.
7. In the event that any glint, glare, or flash affecting the safety of air navigation occurs as a result of project operation, upon notification to the airport operator of an event, the airport operator shall notify the project operator in writing. Within 30 days of written notice, the project operator shall be required to promptly take all measures necessary to eliminate such glint, glare, or flash. An “event” includes any situation that results in an accident, incident, “near-miss,” or specific safety complaint regarding an in-flight experience to the airport operator or to federal, state, or county authorities responsible for the safety of air navigation. The project operator shall work with the airport operator to prevent recurrence of the incidence. Suggested measures may include, but are not limited to, changing the orientation and/or tilt of the source, covering the source at the time of day when events of glare occur, or wholly removing the source to diminish or eliminate the source of the glint, glare, or flash. For each such event made known to the project operator, the necessary remediation shall only be considered to have been fulfilled when the airport operator states in writing that the situation has been remediated to the airport operator’s satisfaction.
8. In the event that any electrical interference affecting the safety of air navigation occurs as a result of project operation, upon notification to the airport operator of an event, the airport operator shall notify the project operator in writing. Within 30 days of written notice, the project operator shall be required to promptly take all measures necessary to eliminate such interference. An “event” includes any situation that results in an accident, incident, “near-miss,” report by airport personnel, or specific safety complaint to the airport operator or to federal, state, or county authorities responsible for the safety of air navigation. The project operator shall work with the airport operator to prevent recurrence of the event. For each such event made known to the project operator, the necessary remediation shall only be considered to have been fulfilled when the airport operator states in writing that the situation has been remediated to the airport operator’s satisfaction.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



FORGESOLAR GLARE ANALYSIS

Project: **MRVP-30**

Proposed 104kW PV rooftop system near March AFB in Moreno Valley, CA.

Site configuration: **Flight Path Analysis**

Analysis conducted by Lawrence Lawlor (lt@sunworksusa.com) at 19:22 on 29 Mar, 2021.

U.S. FAA 2013 Policy Adherence

The following table summarizes the policy adherence of the glare analysis based on the 2013 U.S. Federal Aviation Administration Interim Policy 78 FR 63276. This policy requires the following criteria be met for solar energy systems on airport property:

- No "yellow" glare (potential for after-image) for any flight path from threshold to 2 miles
- No glare of any kind for Air Traffic Control Tower(s) ("ATCT") at cab height.
- Default analysis and observer characteristics (see list below)

ForgeSolar does not represent or speak officially for the FAA and cannot approve or deny projects. Results are informational only.

COMPONENT	STATUS	DESCRIPTION
Analysis parameters	PASS	Analysis time interval and eye characteristics used are acceptable
2-mile flight path(s)	PASS	Flight path receptor(s) do not receive yellow glare
ATCT(s)	PASS	Receptor(s) marked as ATCT do not receive glare

Default glare analysis parameters and observer eye characteristics (for reference only):

- Analysis time interval: 1 minute
- Ocular transmission coefficient: 0.5
- Pupil diameter: 0.002 meters
- Eye focal length: 0.017 meters
- Sun subtended angle: 9.3 milliradians

FAA Policy 78 FR 63276 can be read at <https://www.federalregister.gov/d/2013-24729>

SITE CONFIGURATION

Analysis Parameters

DNI: peaks at 1,000.0 W/m²
Time interval: 1 min
Ocular transmission coefficient: 0.5
Pupil diameter: 0.002 m
Eye focal length: 0.017 m
Sun subtended angle: 9.3 mrad
Site Config ID: 51748.8901



PV Array(s)

Name: PV Array 1
Axis tracking: Fixed (no rotation)
Tilt: 10.0°
Orientation: 182.0°
Rated power: 104.0 kW
Panel material: Smooth glass without AR coating
Reflectivity: Vary with sun
Slope error: correlate with material



Vertex	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
1	33.915051	-117.272082	1561.10	29.00	1590.10
2	33.915049	-117.271659	1561.34	28.76	1590.10
3	33.914635	-117.271672	1562.85	27.25	1590.10
4	33.914636	-117.272088	1560.08	30.02	1590.10

Flight Path Receptor(s)

Name: C/KC Rwy 14 Base

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.922394	-117.325047	1500.07	1500.07	3000.15
Two-mile	33.931244	-117.309014	1500.07	1500.07	3000.15

Name: C/KC Rwy 14 Crosswind

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.821961	-117.228367	1500.07	1500.07	3000.15
Two-mile	33.813147	-117.244350	1500.07	1500.07	3000.15

Name: C/KC Rwy 14 Downwind

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.819225	-117.262269	1500.07	1500.07	3000.15
Two-mile	33.908131	-117.325528	1500.07	1500.07	3000.15

Name: C/KC Rwy 14 Final

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.925156	-117.291061	1500.07	1500.07	3000.15
Two-mile	33.896431	-117.270636	1500.07	0.00	1500.07

Name: C/KC Rwy 14 Upwind

Description: None

Threshold height: 0 ft

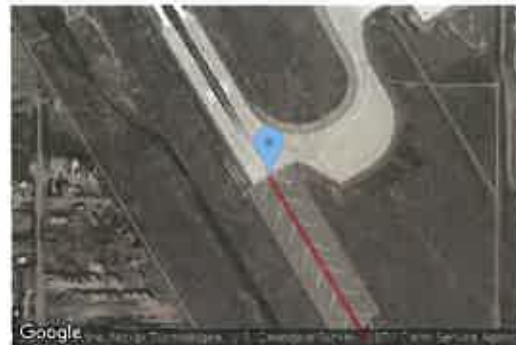
Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.864994	-117.248281	1500.07	0.00	1500.07
Two-mile	33.836269	-117.227869	1500.07	1500.07	3000.15

Name: C/KC Rwy 32 Base

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.813147	-117.244350	1500.07	1500.07	3000.15
Two-mile	33.821961	-117.228367	1500.07	1500.07	3000.15

Name: C/KC Rwy 32 Crosswind

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.931244	-117.309014	1500.07	1500.07	3000.15
Two-mile	33.922394	-117.325047	1500.07	1500.07	3000.15

Name: C/KC Rwy 32 Downwind

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.908131	-117.325528	1500.07	1500.07	3000.15
Two-mile	33.819225	-117.262269	1500.07	1500.07	3000.15

Name: C/KC Rwy 32 Final

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.836269	-117.227869	1500.07	1500.07	3000.15
Two-mile	33.864994	-117.248281	1500.07	0.00	1500.07

Name: C/KC Rwy 32 Upwind

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.896431	-117.270636	1500.07	0.00	1500.07
Two-mile	33.925156	-117.291061	1500.07	1500.07	3000.15

Name: GA Rwy 12 Base

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.910322	-117.264967	1500.07	1300.06	2800.14
Two-mile	33.905592	-117.270622	1500.07	1300.06	2800.14

Name: GA Rwy 12 Crosswind

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.876081	-117.235119	1500.07	1300.06	2800.14
Two-mile	33.880814	-117.229467	1500.07	1300.06	2800.14

Name: GA Rwy 12 Downwind
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.887897	-117.229483	1500.07	1300.06	2800.14
Two-mile	33.910333	-117.256469	1500.07	1300.06	2800.14

Name: GA Rwy 12 Final
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.898508	-117.270608	1500.07	1300.06	2800.14
Two-mile	33.890258	-117.260681	1500.07	0.00	1500.07

Name: GA Rwy 14 Base
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.904833	-117.292903	1500.07	1500.07	3000.15
Two-mile	33.908242	-117.286017	1500.07	1500.07	3000.15

Name: GA Rwy 14 Crosswind

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.848078	-117.243236	1500.07	1500.07	3000.15
Two-mile	33.844669	-117.250119	1500.07	1500.07	3000.15

Name: GA Rwy 14 Downwind

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.846422	-117.258344	1500.07	1500.07	3000.15
Two-mile	33.897972	-117.295011	1500.07	1500.07	3000.15

Name: GA Rwy 14 Final

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.906486	-117.277783	1500.07	1500.07	3000.15
Two-mile	33.896431	-117.270636	1500.07	0.00	1500.07

Name: GA Rwy 14 Upwind
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.864994	-117.248281	1500.07	0.00	1500.07
Two-mile	33.854942	-117.241136	1500.07	1500.07	3000.15

Name: GA Rwy 30 Base
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



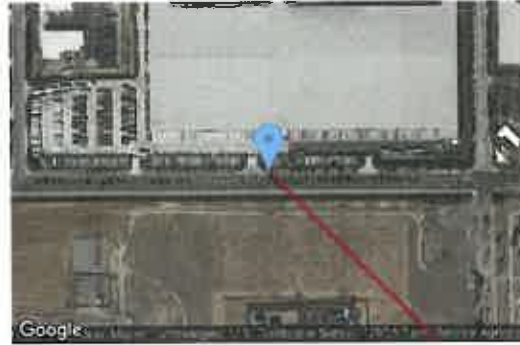
Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.880814	-117.229467	1500.07	1300.06	2800.14
Two-mile	33.876081	-117.235119	1500.07	1300.06	2800.14

Name: GA Rwy 30 Crosswind
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



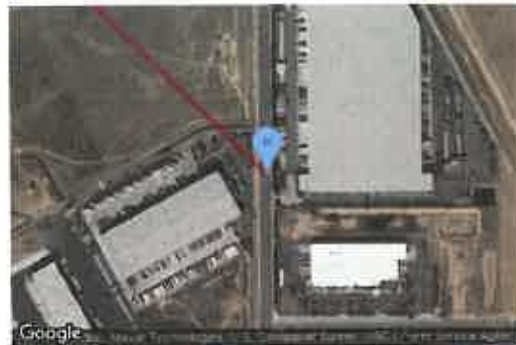
Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.905592	-117.270622	1500.07	1300.06	2800.14
Two-mile	33.910322	-117.264967	1500.07	1300.06	2800.14

Name: GA Rwy 30 Downwind
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.910333	-117.256469	1500.07	1300.06	2800.14
Two-mile	33.887897	-117.229483	1500.07	1300.06	2800.14

Name: GA Rwy 30 Final
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.876069	-117.243611	1500.07	1300.06	2800.14
Two-mile	33.884319	-117.253536	1500.07	0.00	1500.07

Name: GA Rwy 30 Upwind
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.890258	-117.260681	1500.07	0.00	1500.07
Two-mile	33.898508	-117.270608	1500.07	1300.06	2800.14

Name: GA Rwy 32 Base
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



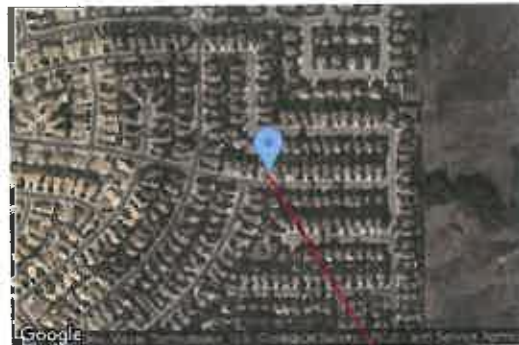
Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.844669	-117.250119	1500.07	1500.07	3000.15
Two-mile	33.848078	-117.243236	1500.07	1500.07	3000.15

Name: GA Rwy 32 Crosswind
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



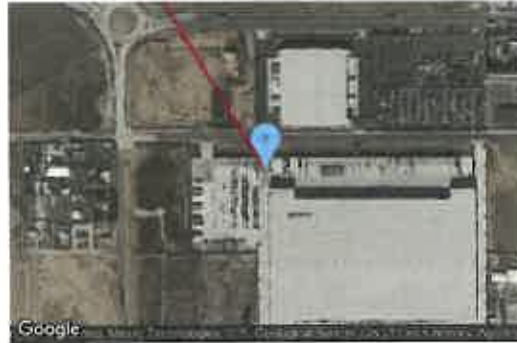
Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.908242	-117.286017	1500.07	1500.07	3000.15
Two-mile	33.904833	-117.292903	1500.07	1500.07	3000.15

Name: GA Rwy 32 Downwind
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.897972	-117.295011	1500.07	1500.07	3000.15
Two-mile	33.846422	-117.258344	1500.07	1500.07	3000.15

Name: GA Rwy 32 Final
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.854942	-117.241136	1500.07	1500.07	3000.15
Two-mile	33.864994	-117.248281	1500.07	0.00	1500.07

Name: GA Rwy R32 Upwind
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.896431	-117.270636	1500.07	0.00	1500.07
Two-mile	33.906486	-117.277783	1500.07	1500.07	3000.15

Name: OHead Rwy 14 Downwind
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.863564	-117.293808	1500.07	2000.10	3500.17
Two-mile	33.908131	-117.325528	1500.07	2000.10	3500.17

Name: OHead Rwy 14 Final

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.925156	-117.291061	1500.07	2000.10	3500.17
Two-mile	33.896431	-117.270636	1500.07	0.00	1500.07

Name: OHead Rwy 14 Initial

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.968036	-117.322128	1500.07	2000.10	3500.17
Two-mile	33.880706	-117.259453	1500.07	2000.10	3500.17

Name: OHead Rwy 32 Downwind

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

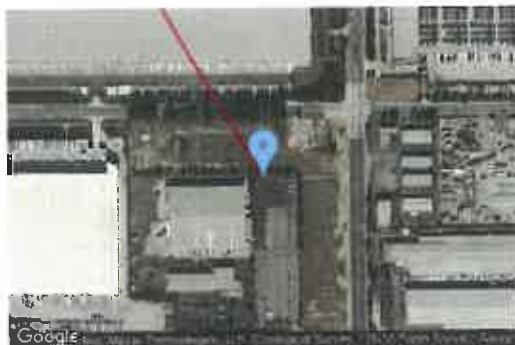
Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.863564	-117.293808	1500.07	2000.10	3500.17
Two-mile	33.819225	-117.262269	1500.07	2000.10	3500.17

Name: OHead Rwy 32 Final
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.836269	-117.227869	1500.07	2000.10	3500.17
Two-mile	33.864994	-117.248281	1500.07	0.00	1500.07

Name: OHead Rwy 32 Initial
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.793375	-117.196878	1500.07	2000.10	3500.17
Two-mile	33.880706	-117.259453	1500.07	2000.10	3500.17

Name: Rwy 12 Upwind (GA)
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.884319	-117.253536	1500.07	0.00	1500.07
Two-mile	33.878069	-117.243611	1500.07	1300.06	2800.14

Discrete Observation Receptors

Name	ID	Latitude (°)	Longitude (°)	Elevation (ft)	Height (ft)
1-ATCT	1	33.891564	-117.251171	1508.07	118.01

Map image of 1-ATCT



GLARE ANALYSIS RESULTS

Summary of Glare

PV Array Name	Tilt (°)	Orient (°)	"Green" Glare	"Yellow" Glare	Energy
			min	min	kWh
PV Array 1	10.0	182.0	2,298	0	232,700.0

Total annual glare received by each receptor

Receptor	Annual Green Glare (min)	Annual Yellow Glare (min)
C/KC Rwy 14 Base	0	0
C/KC Rwy 14 Crosswind	0	0
C/KC Rwy 14 Downwind	0	0
C/KC Rwy 14 Final	0	0
C/KC Rwy 14 Upwind	0	0
C/KC Rwy 32 Base	0	0
C/KC Rwy 32 Crosswind	168	0
C/KC Rwy 32 Downwind	0	0
C/KC Rwy 32 Final	0	0
C/KC Rwy 32 Upwind	799	0
GA Rwy 12 Base	0	0

Receptor	Annual Green Glare (min)	Annual Yellow Glare (min)
GA Rwy 12 Crosswind	0	0
GA Rwy 12 Downwind	0	0
GA Rwy 12 Final	0	0
GA Rwy 14 Base	0	0
GA Rwy 14 Crosswind	0	0
GA Rwy 14 Downwind	0	0
GA Rwy 14 Final	0	0
GA Rwy 14 Upwind	0	0
GA Rwy 30 Base	0	0
GA Rwy 30 Crosswind	0	0
GA Rwy 30 Downwind	1331	0
GA Rwy 30 Final	0	0
GA Rwy 30 Upwind	0	0
GA Rwy 32 Base	0	0
GA Rwy 32 Crosswind	0	0
GA Rwy 32 Downwind	0	0
GA Rwy 32 Final	0	0
GA Rwy R32 Upwind	0	0
OHead Rwy 14 Downwind	0	0
OHead Rwy 14 Final	0	0
OHead Rwy 14 Initial	0	0
OHead Rwy 32 Downwind	0	0
OHead Rwy 32 Final	0	0
OHead Rwy 32 Initial	0	0
Rwy 12 Upwind (GA) 1-ATCT	0	0

Results for: PV Array 1

Receptor	Green Glare (min)	Yellow Glare (min)
C/KC Rwy 14 Base	0	0
C/KC Rwy 14 Crosswind	0	0
C/KC Rwy 14 Downwind	0	0
C/KC Rwy 14 Final	0	0
C/KC Rwy 14 Upwind	0	0
C/KC Rwy 32 Base	0	0
C/KC Rwy 32 Crosswind	168	0
C/KC Rwy 32 Downwind	0	0
C/KC Rwy 32 Final	0	0
C/KC Rwy 32 Upwind	799	0

Receptor	Green Glare (min)	Yellow Glare (min)
GA Rwy 12 Base	0	0
GA Rwy 12 Crosswind	0	0
GA Rwy 12 Downwind	0	0
GA Rwy 12 Final	0	0
GA Rwy 14 Base	0	0
GA Rwy 14 Crosswind	0	0
GA Rwy 14 Downwind	0	0
GA Rwy 14 Final	0	0
GA Rwy 14 Upwind	0	0
GA Rwy 30 Base	0	0
GA Rwy 30 Crosswind	0	0
GA Rwy 30 Downwind	1331	0
GA Rwy 30 Final	0	0
GA Rwy 30 Upwind	0	0
GA Rwy 32 Base	0	0
GA Rwy 32 Crosswind	0	0
GA Rwy 32 Downwind	0	0
GA Rwy 32 Final	0	0
GA Rwy R32 Upwind	0	0
OHead Rwy 14 Downwind	0	0
OHead Rwy 14 Final	0	0
OHead Rwy 14 Initial	0	0
OHead Rwy 32 Downwind	0	0
OHead Rwy 32 Final	0	0
OHead Rwy 32 Initial	0	0
Rwy 12 Upwind (GA)	0	0
1-ATCT	0	0

Flight Path: C/KC Rwy 14 Base

0 minutes of yellow glare

0 minutes of green glare

Flight Path: C/KC Rwy 14 Crosswind

0 minutes of yellow glare

0 minutes of green glare

Flight Path: C/KC Rwy 14 Downwind

0 minutes of yellow glare

0 minutes of green glare

Flight Path: C/KC Rwy 14 Final

0 minutes of yellow glare
0 minutes of green glare

Flight Path: C/KC Rwy 14 Upwind

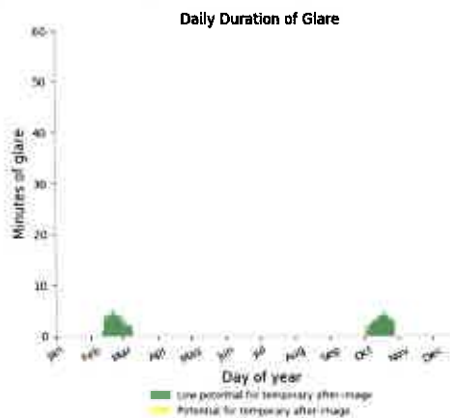
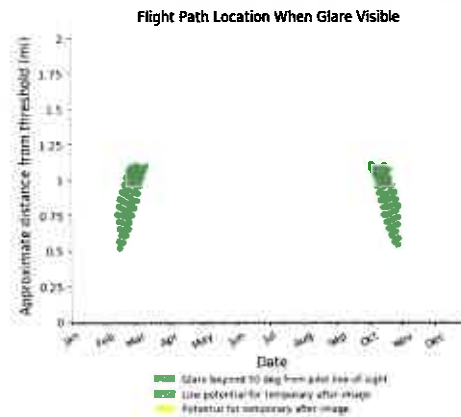
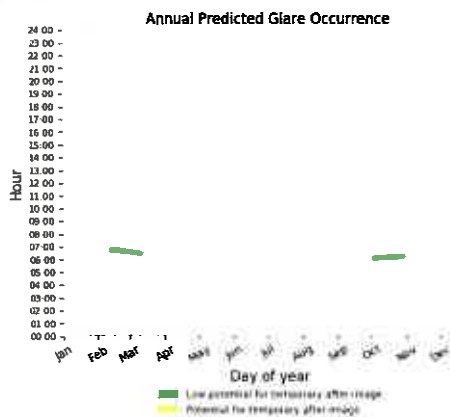
0 minutes of yellow glare
0 minutes of green glare

Flight Path: C/KC Rwy 32 Base

0 minutes of yellow glare
0 minutes of green glare

Flight Path: C/KC Rwy 32 Crosswind

0 minutes of yellow glare
168 minutes of green glare



Flight Path: C/KC Rwy 32 Downwind

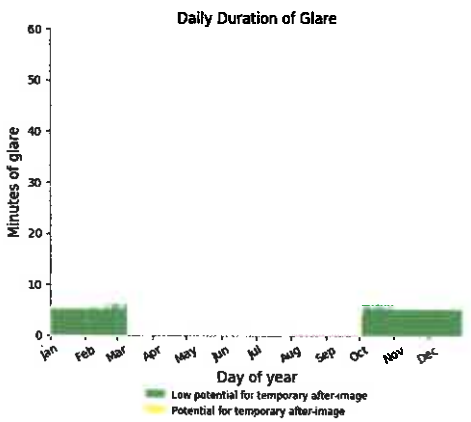
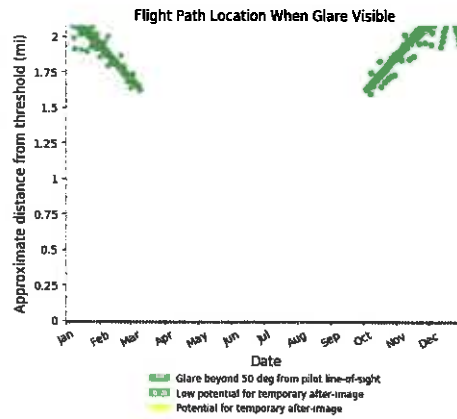
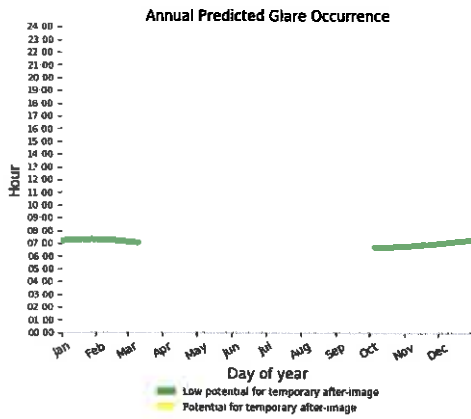
0 minutes of yellow glare
0 minutes of green glare

Flight Path: C/KC Rwy 32 Final

0 minutes of yellow glare
0 minutes of green glare

Flight Path: C/KC Rwy 32 Upwind

0 minutes of yellow glare
799 minutes of green glare



Flight Path: GA Rwy 12 Base

0 minutes of yellow glare
0 minutes of green glare

Flight Path: GA Rwy 12 Crosswind

0 minutes of yellow glare

0 minutes of green glare

Flight Path: GA Rwy 12 Downwind

0 minutes of yellow glare

0 minutes of green glare

Flight Path: GA Rwy 12 Final

0 minutes of yellow glare

0 minutes of green glare

Flight Path: GA Rwy 14 Base

0 minutes of yellow glare

0 minutes of green glare

Flight Path: GA Rwy 14 Crosswind

0 minutes of yellow glare

0 minutes of green glare

Flight Path: GA Rwy 14 Downwind

0 minutes of yellow glare

0 minutes of green glare

Flight Path: GA Rwy 14 Final

0 minutes of yellow glare

0 minutes of green glare

Flight Path: GA Rwy 14 Upwind

0 minutes of yellow glare

0 minutes of green glare

Flight Path: GA Rwy 30 Base

0 minutes of yellow glare

0 minutes of green glare

Flight Path: GA Rwy 30 Crosswind

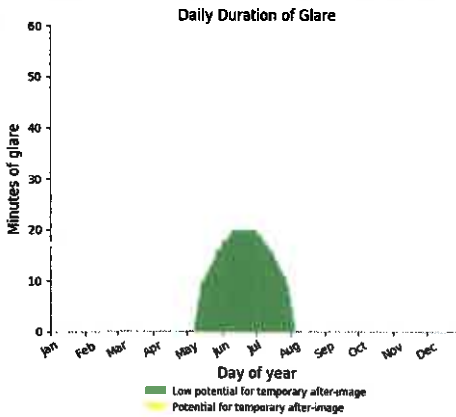
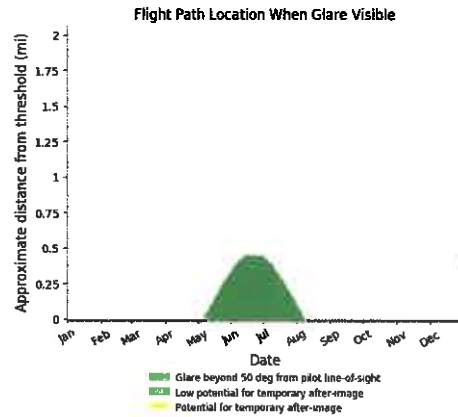
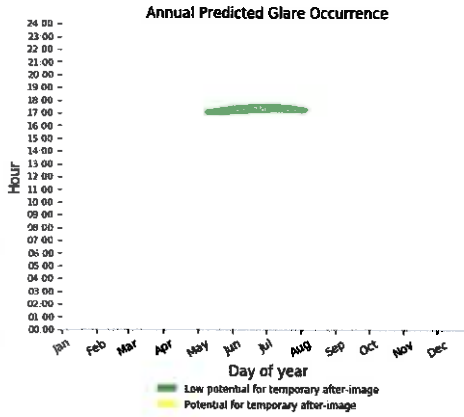
0 minutes of yellow glare

0 minutes of green glare

Flight Path: GA Rwy 30 Downwind

0 minutes of yellow glare

1331 minutes of green glare



Flight Path: GA Rwy 30 Final

0 minutes of yellow glare

0 minutes of green glare

Flight Path: GA Rwy 30 Upwind

0 minutes of yellow glare

0 minutes of green glare

Flight Path: GA Rwy 32 Base

0 minutes of yellow glare

0 minutes of green glare

Flight Path: GA Rwy 32 Crosswind

0 minutes of yellow glare

0 minutes of green glare

Flight Path: GA Rwy 32 Downwind

0 minutes of yellow glare

0 minutes of green glare

Flight Path: GA Rwy 32 Final

0 minutes of yellow glare

0 minutes of green glare

Flight Path: GA Rwy R32 Upwind

0 minutes of yellow glare

0 minutes of green glare

Flight Path: OHead Rwy 14 Downwind

0 minutes of yellow glare

0 minutes of green glare

Flight Path: OHead Rwy 14 Final

0 minutes of yellow glare

0 minutes of green glare

Flight Path: OHead Rwy 14 Initial

0 minutes of yellow glare

0 minutes of green glare

Flight Path: OHead Rwy 32 Downwind

0 minutes of yellow glare

0 minutes of green glare

Flight Path: OHead Rwy 32 Final

0 minutes of yellow glare

0 minutes of green glare

Flight Path: OHead Rwy 32 Initial

0 minutes of yellow glare

0 minutes of green glare

Flight Path: Rwy 12 Upwind (GA)

0 minutes of yellow glare

0 minutes of green glare

Point Receptor: 1-ATCT

0 minutes of yellow glare

0 minutes of green glare

Assumptions

"Green" glare is glare with low potential to cause an after-image (flash blindness) when observed prior to a typical blink response time.

"Yellow" glare is glare with potential to cause an after-image (flash blindness) when observed prior to a typical blink response time.

Times associated with glare are denoted in Standard time. For Daylight Savings, add one hour.

Glare analyses do not account for physical obstructions between reflectors and receptors. This includes buildings, tree cover and geographic obstructions.

Several calculations utilize the PV array centroid, rather than the actual glare spot location, due to algorithm limitations. This may affect results for large PV footprints. Additional analyses of array sub-sections can provide additional information on expected glare.

The subtended source angle (glare spot size) is constrained by the PV array footprint size. Partitioning large arrays into smaller sections will reduce the maximum potential subtended angle, potentially impacting results if actual glare spots are larger than the sub-array size.

Additional analyses of the combined area of adjacent sub-arrays can provide more information on potential glare hazards. (See previous point on related limitations.)

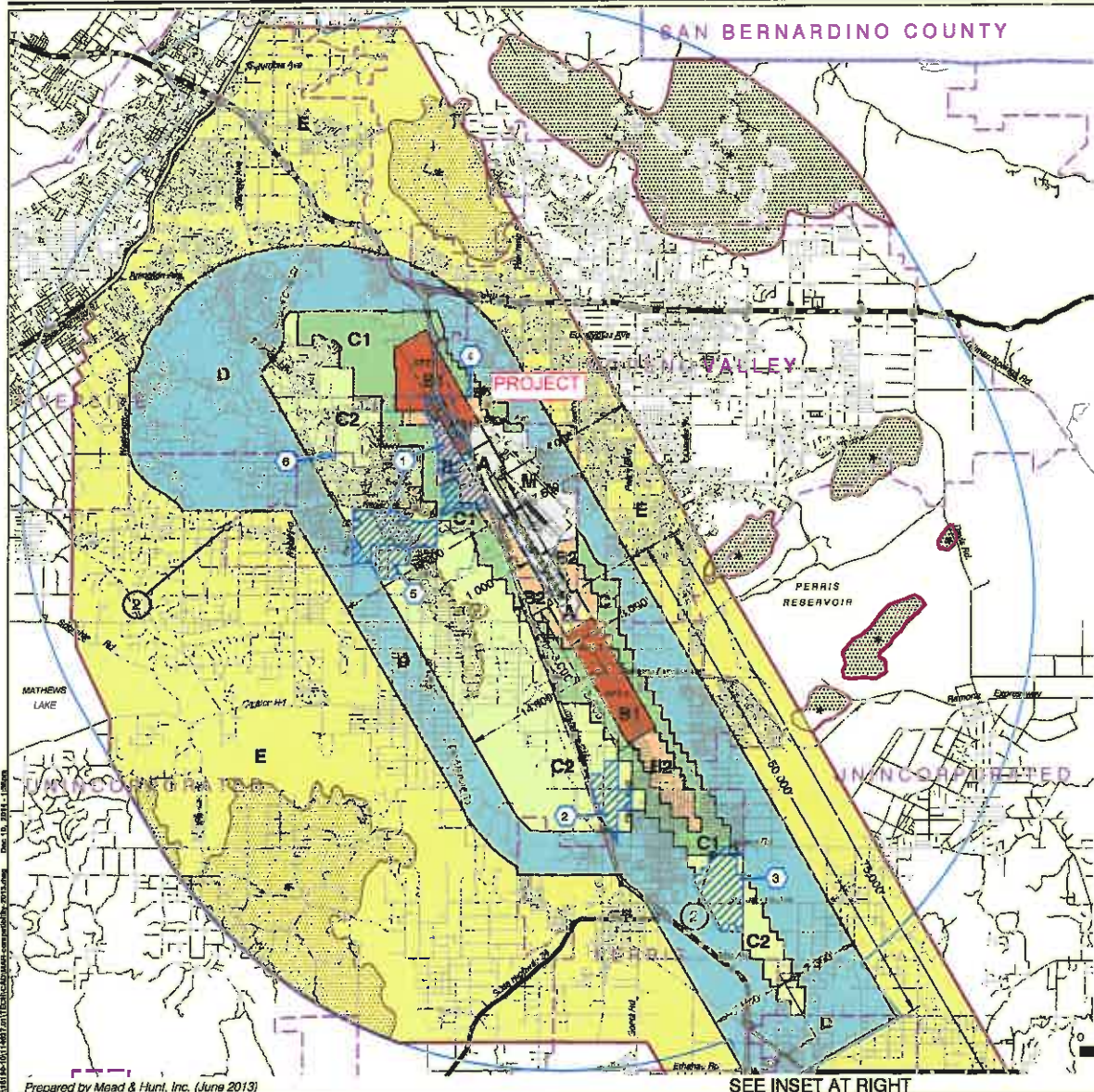
Glare locations displayed on receptor plots are approximate. Actual glare-spot locations may differ.

Glare vector plots are simplified representations of analysis data. Actual glare emanations and results may differ.

The glare hazard determination relies on several approximations including observer eye characteristics, angle of view, and typical blink response time. Actual results and glare occurrence may differ.

Hazard zone boundaries shown in the Glare Hazard plot are an approximation and visual aid based on aggregated research data. Actual ocular impact outcomes encompass a continuous, not discrete, spectrum.

Refer to the Help page at www.forgesolar.com/help/ for assumptions and limitations not listed here.



LEGEND

Compatibility Zones

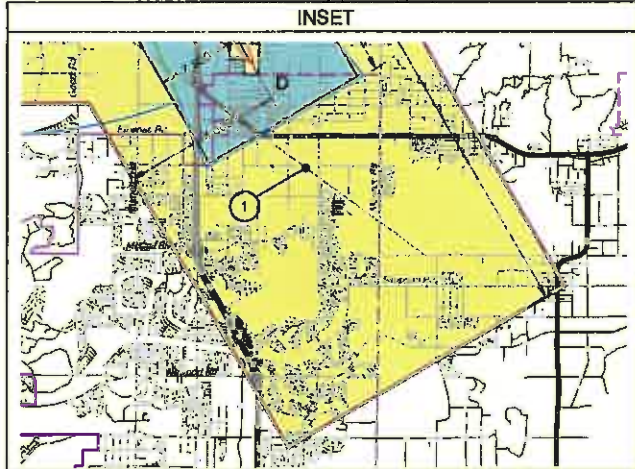
- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

- 1 Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL.
- 2 Point at which departing aircraft typically reach 3,000 feet above runway end.

- March JPA: March Business Center/Meridian
- Perris: Harvest Landing
- Perris: Park West
- Moreno Valley: Affordable Housing
- March JPA: Ben Clark Training Center
- Riverside: Ridge Crest Subdivision



**Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)**

Note:
All dimensions are measured from runway ends and centerlines.



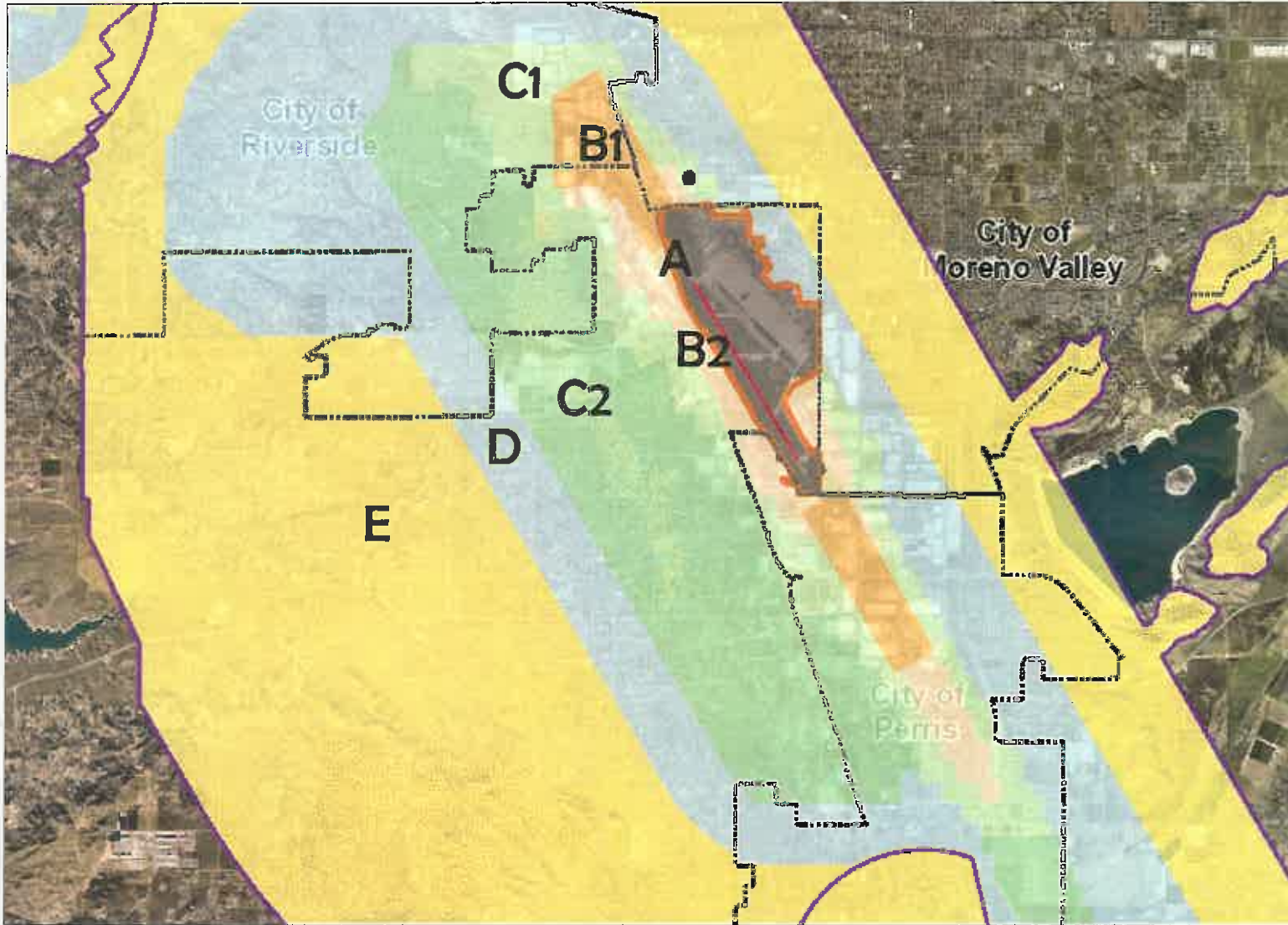
Base map source: County of Riverside 2013

Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map MA-1
Compatibility Map
March Air Reserve Base / Inland Port Airport

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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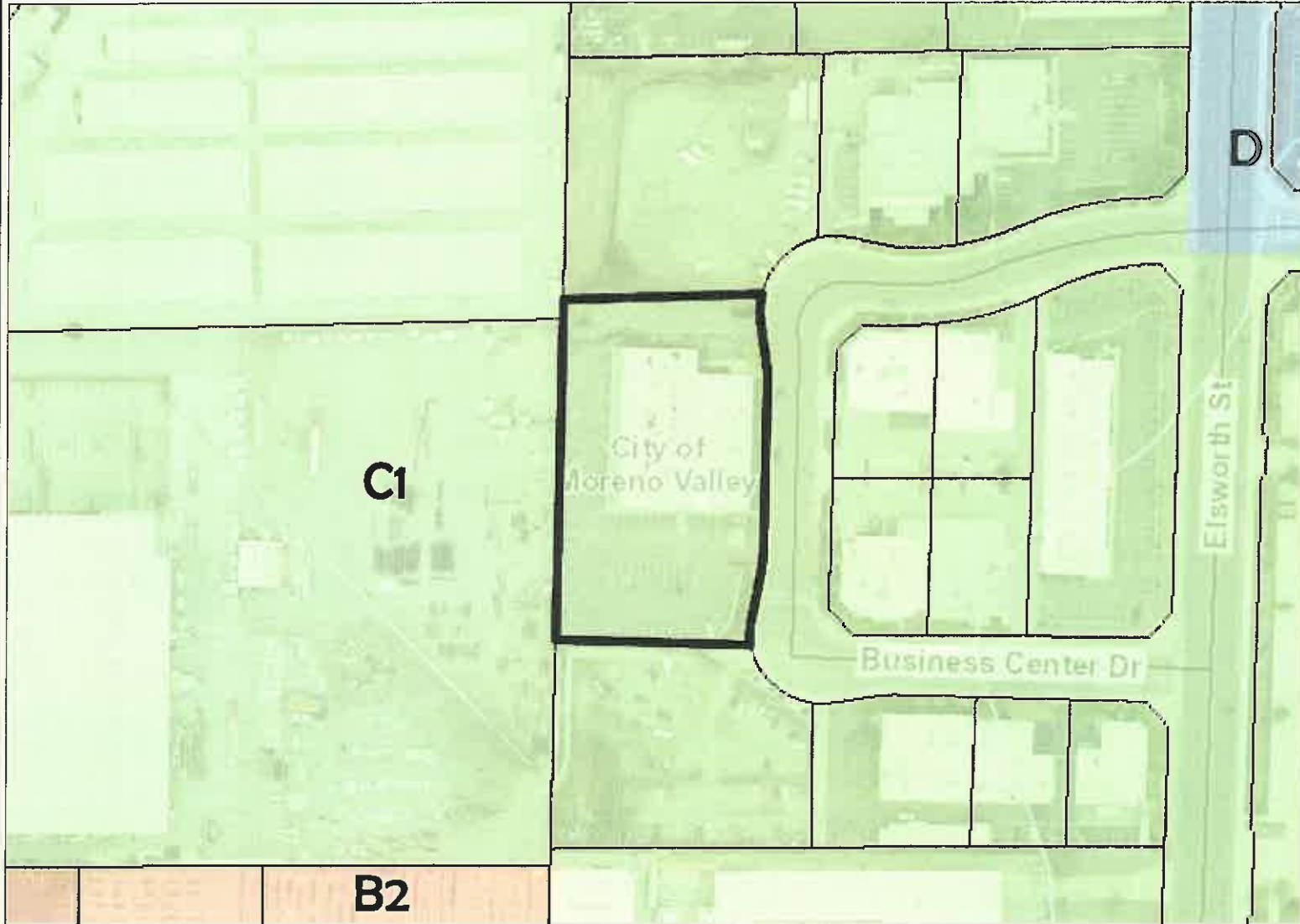


REPORT PRINTED ON... 3/8/2021 10:27:13 AM

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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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Notes

Map My County Map



- Legend**
- Parcels
 - County Centerlines
 - Blue line Streams
 - City Areas
 - World Street Map



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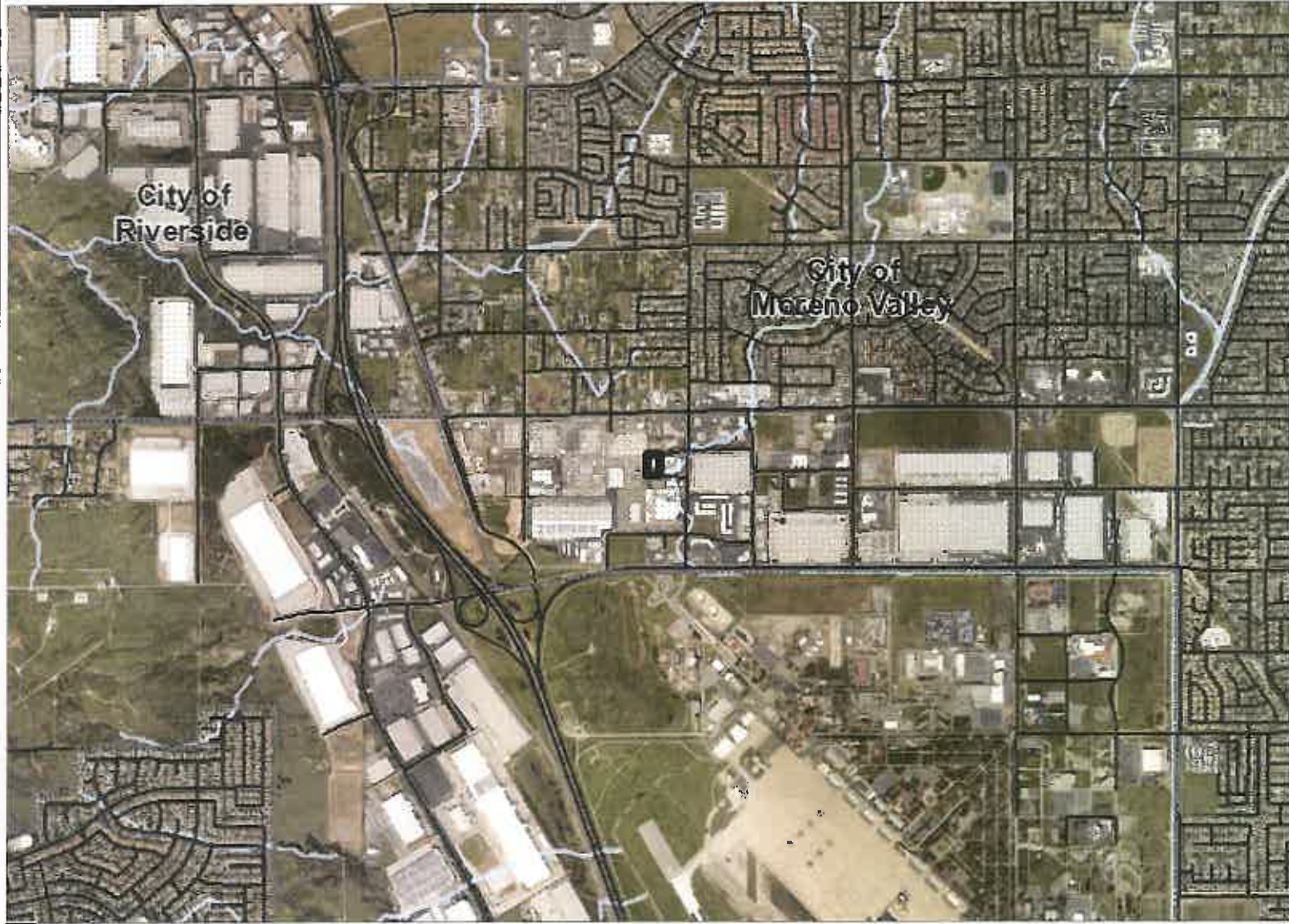


REPORT PRINTED ON... 3/8/2021 10:37:32 AM

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Notes

Map My County Map



Legend

- County Centerlines
- Blue Line Streams
- City Areas
- World Street Map

Notes



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Map My County Map



Legend

- County Centerlines
- Blue Line Streams
- City Areas
- World Street Map

Notes



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Map My County Map



Legend

- Parcels
- County Centerline Names
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map

Notes

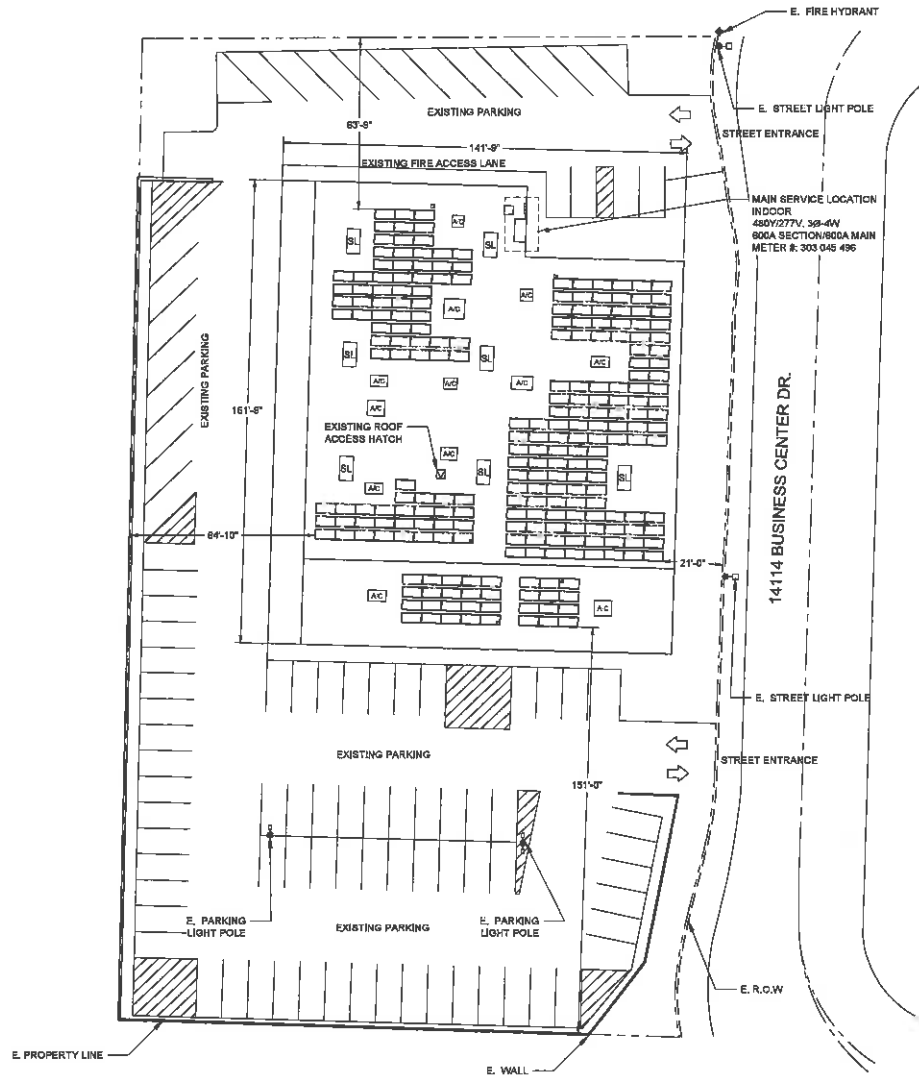


0 188 376 Feet

IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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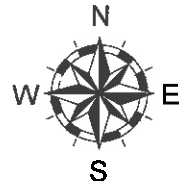
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SITE PLAN
SCALE: 1" = 20'

SITE NOTES

- 1. SOLAR PHOTOVOLTAIC SYSTEMS SHALL BE INSTALLED IN ACCORDANCE WITH SECTIONS 1024.2 THROUGH 1024.4, AND THE CALIFORNIA BUILDING CODE OR CALIFORNIA RESIDENTIAL CODE. THE ELECTRICAL PORTION OF SOLAR PV SYSTEMS SHALL BE INSTALLED IN ACCORDANCE WITH THE CALIFORNIA ELECTRICAL CODE.
- ROOF PATHWAYS SHALL BE OVER AREAS CAPABLE OF SUPPORTING FIRE FIGHTERS ACCESSING THE ROOF AND BE LOCATED IN AREAS WITH MINIMAL OBSTRUCTIONS, SUCH AS VENT PIPES, CONDENSATE OR MECHANICAL EQUIPMENT.
- LOCATIONS OF DC CONDUCTORS, CIRCUIT, WIRING SYSTEMS, AND RACEWAYS FOR PHOTOVOLTAIC SYSTEMS SHALL BE LOCATED AS CLOSE AS POSSIBLE TO THE EDGE ON TOP OR VALLEY AND FROM THE 4" UP OR VALLEY AS NEARLY AS POSSIBLE TO AN OUTSIDE WALL TO REDUCE TOP HAZARDS AND MAXIMIZE VENTILATION OPPORTUNITIES. CONDUIT RINGS BETWEEN SUB AREAS AND TO DC CONSUMER BOXES SHALL BE INSTALLED IN A MANNER THAT MINIMIZES THE TOTAL AMOUNT OF CONDUIT ON THE ROOF BY TAKING THE SHORTEST PATH FROM THE ARRAY TO THE DC CONSUMER BOX. THE DC CONSUMER BOXES SHALL BE LOCATED SUCH THAT THE CONDUIT RINGS ARE MINIMIZED IN THE PATHWAYS BETWEEN ARRAYS. DC WIRING SHALL BE INSTALLED IN METALLIC CONDUIT OR RACEWAYS WHEN LOCATED WITHIN ENCLOSED SPACES BY A BENDING CONDUIT SHALL RUN ALONG THE BOTTOM OF LAND BEARING MEMBERS.
- 2. THERE SHALL BE A MINIMUM 8'-0" FOOT WIDE CLEAR PERIMETER AROUND THE EDGES OF THE ROOF.
- EXCEPTION: WHERE EITHER EDGE OF THE BUILDING IS 20 FT OR LESS, THE CLEAR PERIMETER AROUND THE EDGES OF THE ROOF SHALL BE PERMITTED TO BE REDUCED TO A MINIMUM WIDTH OF 4 FEET.
- 3. INTERIOR PATHWAYS, INTERIOR PATHWAYS SHALL BE PROVIDED BETWEEN ARRAY SECTIONS TO MEET THE FOLLOWING REQUIREMENTS:
 1. PATHWAYS SHALL BE PROVIDED AT INTERVALS NOT GREATER THAN 150 FT THROUGHOUT THE LENGTH AND WIDTH OF THE ROOF.
 2. A PATHWAY NOT LESS THAN 4 FT WIDE IN A STRAIGHT LINE TO ROOF STANDARDS OR VENTILATION HATCHES.
 3. A PATHWAY NOT LESS THAN 4 FT WIDE AROUND ROOF ACCESS HATCHES, WITH NOT FEWER THAN ONE SUCH PATHWAY TO A PARAPET OR ROOF EDGE.
- 4. SMOKE VENTILATION, THE SOLAR INSTALLATION SHALL BE DESIGNED TO MEET THE FOLLOWING REQUIREMENTS:
 1. WHERE NONMANUALLY-OPERATED SMOKE AND HEAT VENTS OCCUR, A PATHWAY NOT LESS THAN 4 FT WIDE SHALL BE PROVIDED BORDERING ALL SIDES.
 2. SMOKE VENTILATION OPTIONS BETWEEN ARRAY SECTIONS SHALL BE ONE OF THE FOLLOWING:
 - 2.1 A PATHWAY NOT LESS THAN 8 FT WIDE.
 - 2.2 WIDE, QUANTITY-OPERATED DISPOUT SMOKE AND HEAT VENTS OCCUR, A PATHWAY NOT LESS THAN 4 FT WIDE ON ANY FLOOR THAN ONE SIDE.
 - 2.3 A PATHWAY NOT LESS THAN 4 FT WIDE BORDERING 4 FT BY 8 FT WIDE OPENINGS EVERY 20 FT ON ALL SURROUNDING SIDES OF THE PATHWAY.



LEGAL DESCRIPTION

ABB: 257-210-026
 STATUS ADDRESS: 14114 BUSINESS CENTER DR.
 USE TYPE: COMMERCIAL - OFFICE
 BOOK: 411
 PAGE: 28
 TRACT #: 1026
 LOT: 1026
 YEAR BUILT: 1999
 PARCEL SIZE: 1.62 ACRES



CONTRACTOR
SUNWORKS
 1030 WINDING CREEK RD, STE. 100
 ROSEVILLE, CA 95678
 CSLB: #441690 / C-10, C-46
 (916) 408-6900

I HEREBY CERTIFY THAT THE WORK PROPOSED TO BE DONE ON THESE PLANS IS IN CONFORMANCE WITH ALL CODES AND ORDINANCES OF THE CITY OF FRESNO AND FURTHER, IF CHANGES OR ERRORS ARE DISCOVERED, I UNDERTAKING THAT THE WORK PROPOSED WILL BE REQUIRED TO COMPLY WITH THE CODES AND ORDINANCES OF THE CITY OF FRESNO PRIOR TO FINAL BUILDING PERMITTING.

SIGNATURE _____ DATE _____ STATE LICENSE NO. #441690 / C-10, C-46

Property Owned & Managed by:
MVRP PV
 14114 BUSINESS CENTER DR.
 MORENO VALLEY, CA 92553

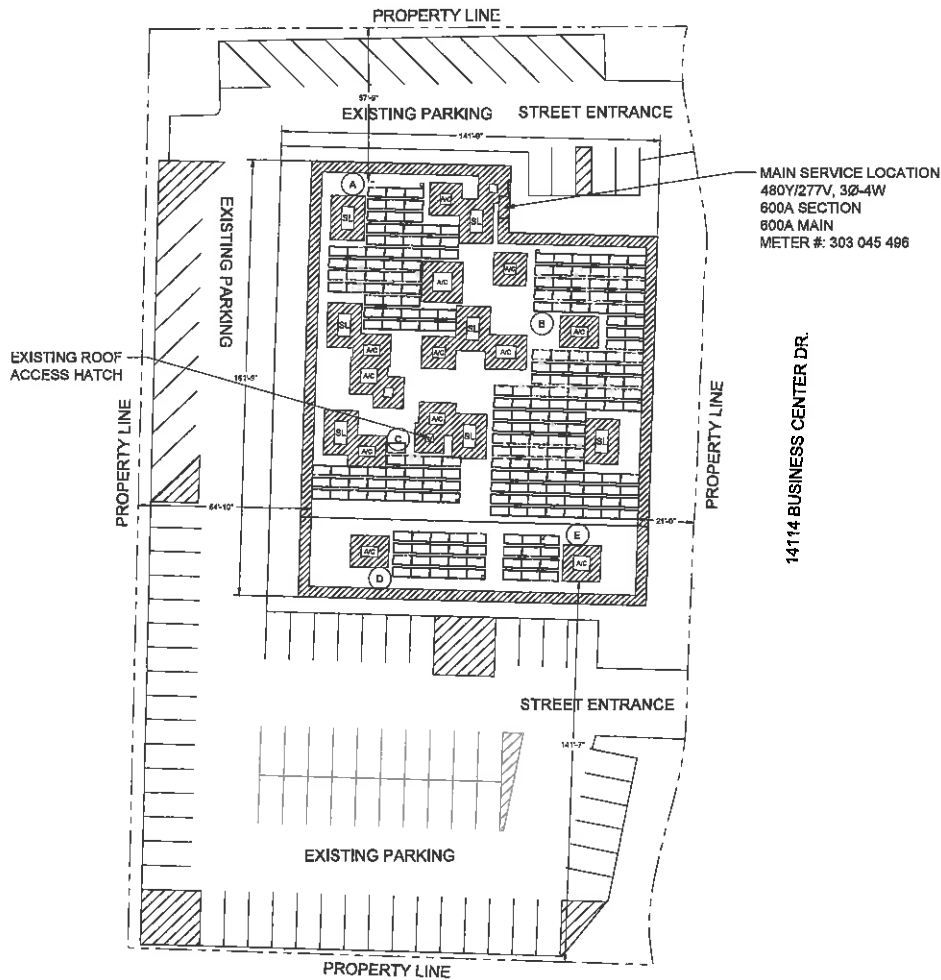
ARCH D (SHEET) PRINT PAPER SIZE

NO.	DATE	REVISIONS	ELECT.	STRUC.
1	1/25/2020	PLANNING	A.L.	-
2	1/25/2020	REVISION 1	A.L.	-
3	1/25/2020	REVISION 2	A.L.	-
4	1/25/2020	REVISION 3	A.L.	-
5	1/25/2020	REVISION 4	A.L.	-
6	1/25/2020	SITE PLAN	F.L.	-

SYSTEM INFO:
 TRACT #: 1026
 TOTAL SYSTEM SIZE: DC STD: 102.65 KW
 TOTAL SYSTEM SIZE: AC STD: 26.48 KW
 (DC) INVERTER: SUN L-063-400W
 (1) SOLARDC SYSTEMS INVERTER

DESCRIPTION:
SITE PLAN

PV 2.1



SITE PLAN
SCALE: 3/8" = 1'-0"

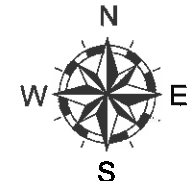
ARRAY INFORMATION					
ROOF	A	B	C	D	E
ROOF PITCH	<5°	<5°	<5°	<5°	<5°
ROOF MATERIAL	ROLLED COMP.	ROLLED COMP.	ROLLED COMP.	ROLLED COMP.	ROLLED COMP.
MODULE COUNT	54	127	29	20	12
MODULE AREA	1127.9SQ.FT.	2652.6SQ.FT.	605.7SQ.FT.	417.7SQ.FT.	250.6SQ.FT.
ARRAY TILT	10°	10°	10°	10°	10°
ARRAY AZIMUTH	181°	181°	181°	181°	181°

SITE NOTES

- 1. ROOF ACCESS FOR BUILDINGS OTHER THAN ONE AND TWO DOWLING UNITS SHALL BE PROVIDED WITH A MINIMUM OF 300 FOOT (8') WIDE CLEAR PERIMETER AROUND THE EDGES OF THE ROOF.
EXCEPTION: IF EITHER AXIS OF THE BUILDING IS 250 FEET OR LESS, THERE SHOULD BE A MINIMUM FOUR FEET (4') WIDE CLEAR PERIMETER AROUND THE EDGES OF THE ROOF.
- 2. PHOTOVOLTAIC SYSTEMS SHALL BE MARKED TO IDENTIFY THE MAIN ELECTRICAL SERVICE DISCONNECT. MATERIALS USED FOR MARKING SHALL BE WEATHER RESISTANT AND MEET THE SPECIFICATIONS FOR WEATHER MARKING.
- 3. THE MAIN ELECTRICAL SERVICE DISCONNECT MARKING SHALL BE PLACED ADJACENT TO THE MAIN SERVICE DISCONNECT IN A LOCATION CLEARLY VISIBLE FROM THE LOCATION WHERE THE LEVER IS OPERATED, FOR COMMERCIAL AND RESIDENTIAL BUILDINGS.
- 4. PHOTOVOLTAIC CIRCUIT MARKING SHALL BE PLACED ON ALL INTERIOR AND EXTERIOR PHOTOVOLTAIC DC CONDUIT, RACKWAYS, ENCLOSURES, CABLE ASSEMBLY, AND JUNCTION BOXES. MARKINGS SHALL BE PLACED EVERY 10 FEET, AT TURNING POINTS AND/OR BELOW PENETRATIONS, AND AT ALL PHOTOVOLTAIC CIRCUIT TERMINAL AND JUNCTION BOXES.
- 5. ELECTRICAL SYSTEM MAIN SERVICE DISCONNECT, PHOTOVOLTAIC CIRCUIT, AND REMOTE ELECTRICAL DISCONNECT MARKINGS SHALL COMPLY WITH THE FORMAT AND CONTENT SET FORTH IN NATIONAL ELECTRICAL CODE.
- 6. SOLAR PHOTOVOLTAIC POWER SYSTEMS SHALL BE INSTALLED IN ACCORDANCE WITH SECTIONS 902.1.1 THROUGH 902.11.2, THE CALIFORNIA BUILDING CODE, OR CALIFORNIA MECHANICAL CODE, AND CALIFORNIA ELECTRICAL CODE.
-FIRE CODE 902.1.
- 7. PATHWAYS SHALL BE OVER AREAS CAPABLE OF SUPPORTING FIREWORKS. ACCESSING THE ROOF, PATHWAYS SHALL BE LOCATED IN AREAS WITH ANIMAL OBSTRUCTIONS SUCH AS WIRE PAPER, CONDUIT OR MECHANICAL EQUIPMENT.
-FIRE CODE 902.11.1.
- 8. ROOF ACCESS POINTS SHALL BE LOCATED IN AREAS THAT DO NOT REQUIRE THE PLACEMENT OF DOWNWARD LABELS OVER OPENINGS SUCH AS WINDOWS OR DOORS, AND LOCATED AT STRONG POINTS OF BUILDING CONSTRUCTION IN LOCATIONS WHERE THE ACCESS POINTS DOES NOT CONFLICT WITH OVERHEAD OBSTRUCTIONS SUCH AS TREE LIMBS, WIRES OF LINES.
-FIRE CODE 902.11.1.1.

FIRE CODE (CHAPTER 8 OF CALIFORNIA FIRE CODE)

- 902.11.1.1 ACCESS. THERE SHALL BE A MINIMUM 6'-FOOT-WIDE (1829 MM) CLEAR PERIMETER AROUND THE EDGES OF THE ROOF.
EXCEPTION: WHERE EITHER AXIS OF THE BUILDING IS 250 FEET (7620 MM) OR LESS, THERE SHALL BE A MINIMUM 4'-FOOT-WIDE (1219 MM) CLEAR PERIMETER AROUND THE EDGES OF THE ROOF.
- 902.11.2 LOCATIONS OF DC CONDUCTORS, CONDUIT, WIRING SYSTEMS, AND RACKWAYS FOR PHOTOVOLTAIC CIRCUITS SHALL BE LOCATED AS CLOSE AS POSSIBLE TO THE ROOF OR ON OR NEAR VALLEY AND FROM THE HIGH OF VALLEY AS DIRECTLY AS POSSIBLE TO AN OUTSIDE WALL, TO PREVENT TRIP HAZARDS AND HAZARDOUS VENTILATION OPPORTUNITIES. CONDUIT SHALL BE BETWEEN SUN ARAYS AND TO NO CHANGE BOXES SHALL BE INSTALLED IN A MANNER THAT MINIMIZES THE TOTAL AMOUNT OF CONDUIT ON THE ROOF BY TAKING THE SHORTEST PATH FROM THE ARRAY TO THE DC COMBINER BOX. THE DC COMBINER BOXES SHALL BE LOCATED SUCH THAT CONDUIT RACKS ARE INSTALLED IN THE PATHWAYS BETWEEN ARRAYS. DC WIRING SHALL BE INSTALLED BY INSTALLING CONDUIT OR RACKWAYS WITH LOCATED WITHIN ENCLOSED SPACES IN A BUILDING. CONDUIT SHALL RUN ALONG THE BOTTOM OF LONG BEARING MEMBERS.



4FT CLEARANCE



CONTRACTOR
SUNWORKS
1030 WINDING CREEK RD. STE. 100
ROSEVILLE, CA 95678
CSLB: #441890 / C-10, C-46
(816) 409-8900

I HEREBY CERTIFY THAT THE WORK PROPOSED TO BE DONE ON THESE PLANS IS IN CONFORMANCE WITH ALL CODES AND ORDINANCES OF THE CITY OF FREEMO AND FURTHER, IF CONDITIONS OR ERRORS ARE DISCOVERED, I UNDERSTAND THAT THE WORK PERFORMED SHALL BE REQUIRED TO COMPLY WITH THE CODES AND ORDINANCES OF THE CITY OF FREEMO PRIOR TO FINAL INSPECTION.
SIGNATURE _____ DATE _____ STATE LICENSE NO. #441890 / C-10, C-46

Properly Owned & Managed by:
MVRP PV
14114 BUSINESS CENTER DR.
MORENO VALLEY, CA 92553

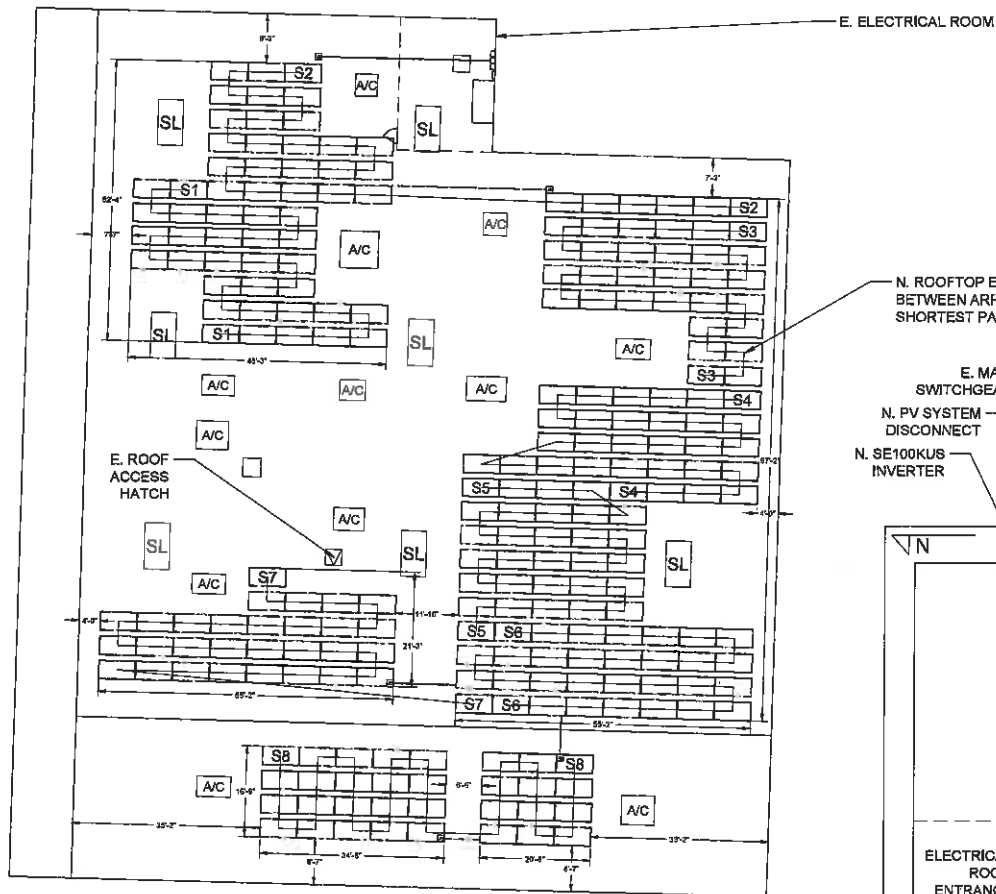
ARCH DWT'S 847 PRINT PAPER SIZE

NO.	DATE	REVISIONS	ELECT.	STRUC.
10/20/2010		PLANNING	AL.	-
10/20/2010		REVISION 1	AL.	-
10/20/2010		REVISION 2	AL.	-
10/14/2010		REVISION 3	AL.	-
10/20/2010		REVISION 4	AL.	-

SYSTEM INFO:
TOTAL SYSTEM SIZE: DC STC: 102.83 kW
TOTAL SYSTEM SIZE: AC CEC: 31.48 kW
(4%) ILLUM. GAINING OVER 1-65% KWH
(1) SOLARWING SOLARIS INVERTERS

DESCRIPTION:
SITE PLAN

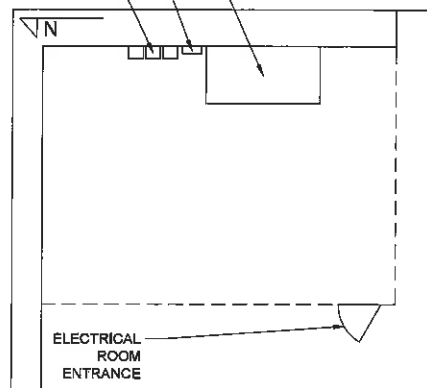
PV 2



PLOT PLAN
SCALE: 3/32" = 1'-0"

N. ROOFTOP EMT CONDUIT
BETWEEN ARRAY SECTIONS
SHORTEST PATH

E. MAIN
SWITCHGEAR
N. PV SYSTEM
DISCONNECT
N. SE100KUS
INVERTER



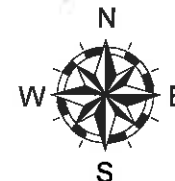
ENLARGED ELECTRICAL ROOM PLAN
SCALE: 1/4" = 1'-0"

BUILDING SPECIFICATIONS

TYPE: COMMERCIAL
STORIES: 1
BUILDING HEIGHT: 25 FT.
SQUARE FOOTAGE: 18,240
ROOF SLOPE: <5°
ROOF TYPE: ROLLED COMP.

MOUNTING SPECIFICATIONS

RACKING SYSTEM: SOLLEGA FASTTRACK 10"
INTERROW SPACING: 13"
MOUNTING TYPE: TILT 10°
TILT LEG HEIGHT: 14"
HEIGHT OF PV MODULE ABOVE ROOF SURFACE: 14"



CONTRACTOR

SUNWORKS
1030 WINDING CREEK RD. STE. 100
ROSEVILLE, CA 95678
CSLB: #441690 / C-10, C-46
(916) 409-8900

THESE PLANS ARE IN COMPLIANCE WITH ALL ORDINANCES AND ORDINANCES OF THE CITY OF PLEASANTON AND ANY OTHER CITY OR JURISDICTION. THE CONTRACTOR SHALL BE RESPONSIBLE TO COMPLY WITH THE CODES AND ORDINANCES OF THE CITY OF PLEASANTON PRIOR TO FINAL BUILDING INSPECTION.

SIGNATURE: _____ DATE: _____ STATE LICENSE NO.: #441690 / C-10, C-46

Property Owned & Managed by:

MVRP PV
14114 BUSINESS CENTER DR.
MORENO VALLEY, CA 92553

ARCH D (24" X 36") PERMANENT PAPER SIZE				
NO.	DATE	REVISIONS	ELECT.	STRUC.
220622		PLANSET	AJ.	-
07/19/2012		REVISION 1	AJ.	-
07/19/2012		REVISION 2	AJ.	-
10/16/2012		REVISION 3	AJ.	-
10/29/2012		REVISION 4	AJ.	-

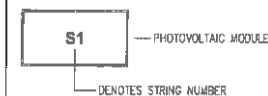
SYSTEM INFO:
TOTAL SYSTEM SIZE: DC STD: 100.00 KW
TOTAL SYSTEM SIZE: AC OCC: 34.46 KW
[S1] 1000V DC BREAK AND L-BOB 400V
[1] SOLAREDGE S1000S INVERTER

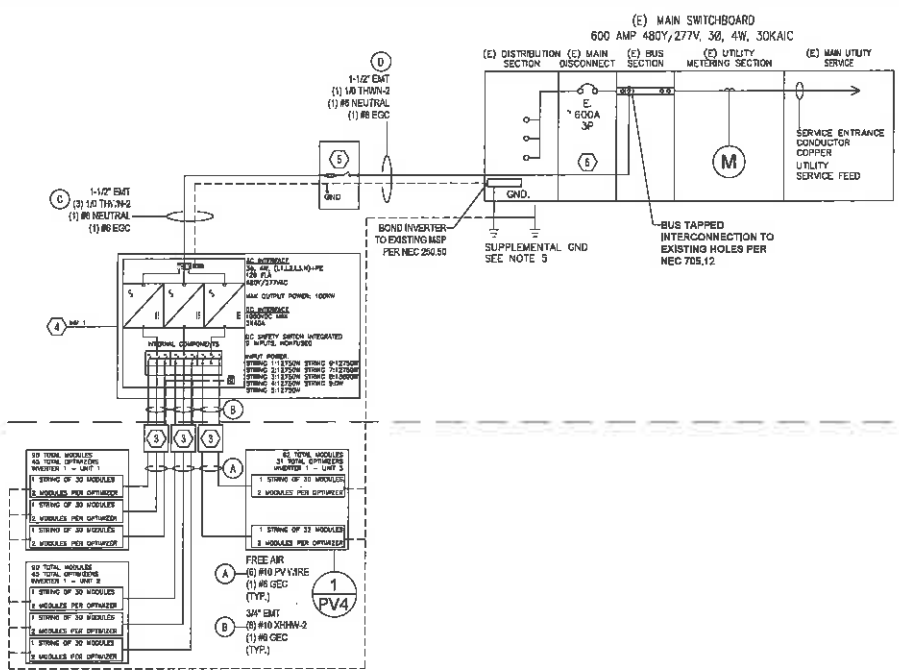
DESCRIPTION:

PLOT PLAN

PV 3

PLAN LEGEND





705.12(A). LINE SIDE CONNECTION
ELECTRIC POWER PRODUCTION SOURCES CONNECTED VIA SUPPLY SIDE OF THE SERVICE DISCONNECTING MEANS PER 250.42(1)(5)
150A @ 600A O.K.

ANY ALTERATIONS TO THE EXISTING MAIN SWITCHBOARD'S MECHANICAL/ELECTRICAL CHARACTERISTICS REQUIRES A FULL PARTY SITE EVALUATION TO RE-CERTIFY THE SWITCHBOARD TO UL STANDARDS.

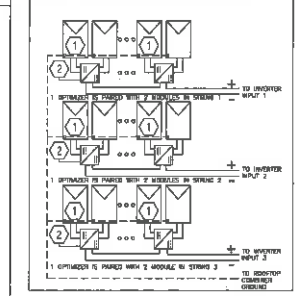
ELECTRICAL ROOM
GROUND FLOOR

ROOFTOP

EQUIPMENT NOTES

- 1. PHOTOVOLTAIC MODULES INCLUDE #12 AWG OUTDOOR RATED WGA CONNECTORS FOR MODULAR INTERCONNECTION. DO NOT REMOVE THE QUICK CONNECTS. OTHERWISE THE MODULAR WARRANTY AND THE UL LISTING WILL BE INVALIDATED.
- 2. 86 AWG BARE COPPER GROUND WILL BE USED AS EQUIPMENT GROUND FOR THE RACKING. USE MODULAR GROUNDING METHODS PER MANUFACTURERS INSTALLATION REQUIREMENTS. THE MODULAR EQUIPMENT GROUND SHALL TERMINATE AT THE INVERTER CABINET.
- 3. INVERTER AREA SHALL BE RATED WITH UL 1741-5A LISTING INCLUDING INTERNAL FIRE-RESISTANT PROTECTION FINISHED WITH 1/2\"/>

1 PV4 MODULE/OPTIMIZER STRING INVERTER 1-2



CONTRACTOR

SUNWORKS
1030 WINDING CREEK RD. STE. 100
ROSEVILLE, CA 95678
CSLB: #441890 / C-10, C-46
(916) 409-6900

I HEREBY CERTIFY THAT THE WORK PROPOSED TO BE DONE ON THESE PLANS IS BY CONTRACTORS WITH ALL LICENSES AND REQUIREMENTS OF THE CITY OF FREEMONT AND FURTHER, IF CHANGES OR ERRORS ARE DISCOVERED, UNDERSTANDING THAT THE WORK PERFORMED WILL BE REQUIRED TO COMPLY WITH THE CODES AND ORDINANCES OF THE CITY OF FREEMONT PRIOR TO FINAL PAYMENT OF THE PROJECT.

SIGNATURE: _____ DATE: _____ STATE LICENSE NO.: #441890 / C-10, C-46



Property Owned & Managed by:

MVPR PV
14114 BUSINESS CENTER DR.
MORENO VALLEY, CA 92553

NO.	DATE	REVISIONS	ELECT.	STRUC.
1	10/20/20	PLANS	A.L.	-
2	10/20/20	REVISION 1	A.L.	-
3	10/20/20	REVISION 2	A.L.	-
4	10/20/20	REVISION 3	A.L.	-
5	10/20/20	REVISION 4	A.L.	-

SYSTEM INFO:

TOTAL SYSTEM SIZE: DC SID: 102.88 KW
TOTAL SYSTEM SIZE: AC SID: 34.46 KW
(2) Q-CELL Q-CELL PV MODULES
(1) SOLAREDGE STRING INVERTERS

DESCRIPTION:
SINGLE LINE DIAGRAM

PV 4

NEW EQUIPMENT SCHEDULE

EQUIPMENT	MANUFACTURER/MODEL	EQUIPMENT DESCRIPTION
1	Q-CELL Q-CELL PVK DUAL L-G8.2 425W	Q-CELL 425W PV MODULE
2	SOLAREDGE P880	SOLAREDGE 880W DUAL MODULE DC/DC POWER OPTIMIZER
3	JUNCTION BOX	NEMA 3R JUNCTION BOX
4	SOLAREDGE SE100KUB	SOLAREDGE 100KW INVERTER WITH INTEGRATED DC DISCONNECT
5	AC DISCONNECT SWITCH	200A, 150A FUSES, 480Y/277V, 3P, 4W, VIEWABLE, LOCKABLE, 30KAIC
6	EXISTING MAIN SERVICE PANEL	600A, MAIN BUSBAR, 480Y/277V, 3P, 4W, 30KAIC

NO.	DESCRIPTION	UNIT	QTY	UNIT PRICE	TOTAL PRICE	MARKET VALUE	NET PRICE	NET TOTAL
1	Q-CELL Q-CELL PVK DUAL L-G8.2 425W	Module	2	100.00	200.00	100.00	100.00	200.00
2	SOLAREDGE P880	Optimizer	2	450.00	900.00	450.00	450.00	900.00
3	JUNCTION BOX	Box	2	50.00	100.00	50.00	50.00	100.00
4	SOLAREDGE SE100KUB	Inverter	1	1000.00	1000.00	1000.00	1000.00	1000.00
5	AC DISCONNECT SWITCH	Switch	1	200.00	200.00	200.00	200.00	200.00
6	EXISTING MAIN SERVICE PANEL	Panel	1	0.00	0.00	0.00	0.00	0.00
TOTAL								

NAME	COUNT	WIRE TYPE	CONDUCTORS	INSULATION	ADDITIONAL COMMENTS	AMOUNT	UNIT PRICE	TOTAL PRICE	MARKET VALUE	NET PRICE	NET TOTAL
A	1	12 AWG	3	THHN	CONDUIT	100	1.50	150.00	100.00	50.00	150.00
B	1	10 AWG	3	THHN	CONDUIT	100	1.50	150.00	100.00	50.00	150.00
C	1	8 AWG	3	THHN	CONDUIT	100	1.50	150.00	100.00	50.00	150.00
TOTAL											

CONTRACTOR

SUNWORKS
1030 WINDING CREEK RD. STE. 100
ROSEVILLE, CA 95678
CSLB: #441880 / C-10, C-46
(916) 409-6900

I HEREBY CERTIFY THAT THE WORK PROPOSED TO BE DONE ON THESE PLANS IS IN COMPLIANCE WITH ALL CODES AND ORDINANCES OF THE AHJ OF CITY OF FREEMO AND FURTHER, IF CHANGES OR ERRORS ARE DISCOVERED, I UNDERSTAND THAT THE WORK PERFORMED WILL BE REQUIRED TO COMPLY WITH THE CODES AND ORDINANCES OF THE AHJ OF CITY OF FREEMO PRIOR TO FINAL BUILDING INSPECTION.

SIGNATURE: _____ DATE: _____ STATE LICENSE NO. #441880 / C-10, C-46

Property Owned & Managed by:

MVRP PV
14114 BUSINESS CENTER DR.
MORENO VALLEY, CA 92553

NO.	DATE	REVISIONS	ELECT.	STRUC.
DESIGNED	PLANS SET	A.L.	-	-
REVISION	REVISION 1	A.L.	-	-
REVISION	REVISION 2	A.L.	-	-
REVISION	REVISION 3	A.L.	-	-
REVISION	REVISION 4	A.L.	-	-

SYSTEM INFO:
TOTAL SYSTEM SIZE: DC DC: 102.85 kW
TOTAL SYSTEM SIZE: AC DC: 114.48 kW
(N) 100% GROUNDING L-NEE 2.45W
(U) SOLARGE SENIORS INVERTERS

DESCRIPTION:
GROUNDING

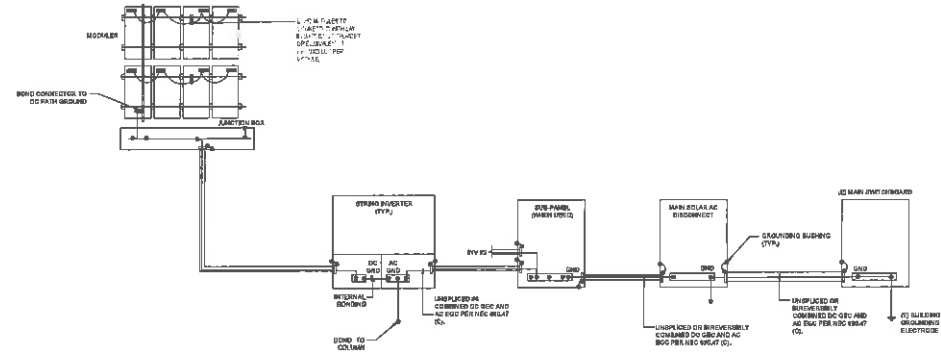
PV 5

OWNER: SUNWORKS	DESIGNER: SUNWORKS	PROJECT: MVRP PV	DWG NO: G0977	DATE: 10/1/14
DATE: 10/1/14	SCALE: 1/8" = 1'-0"	DATE: 10/1/14	SCALE: 1/8" = 1'-0"	DATE: 10/1/14

**Cat #: GBL-4DB
GBL-4DBT**

SCALE: 5 : 1

NO.	DATE	REVISIONS	ELECT.	STRUC.
DESIGNED	PLANS SET	A.L.	-	-
REVISION	REVISION 1	A.L.	-	-
REVISION	REVISION 2	A.L.	-	-
REVISION	REVISION 3	A.L.	-	-
REVISION	REVISION 4	A.L.	-	-



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PV SYSTEM LABELING REQUIREMENTS NOT TO SCALE

- ① COMBINER BOX/ CIRCUITS/ CONDUIT/ COMBINER BOX/ ENCLOSURES/ EMT ENCLOSURES

WARNING
ELECTRICAL SHOCK HAZARD
TERMINALS ON BOTH THE LINE AND LOAD SIDE MAY BE ENERGIZED IN THE OPEN POSITION

WARNING
TURN OFF PHOTO VOLTAGE AC DISCONNECT PRIOR TO WORKING INSIDE PANEL

- ② BUILDING / STRUCTURE
NEC 705.10 - BY MAIN SERVICE PANEL:

CAUTION
POWER TO THIS BUILDING IS ALSO SUPPLIED FROM THE FOLLOWING SOURCES WITH DISCONNECTS LOCATED AS SHOWN.

WITH MAIN SERVICE CENTER DR. MORENO VALLEY, CA 92551

WARNING
TERMINALS ON BOTH THE LINE AND LOAD SIDE MAY BE ENERGIZED IN THE OPEN POSITION

- ⑨ MAIN SERVICE DISCONNECT

WARNING
ELECTRICAL SHOCK HAZARD
TERMINALS ON BOTH THE LINE AND LOAD SIDE MAY BE ENERGIZED IN THE OPEN POSITION

WARNING
TURN OFF PHOTOVOLTAGE AC DISCONNECT PRIOR TO WORKING INSIDE PANEL

CAUTION
PHOTOVOLTAGE SYSTEM CIRCUIT IS BACKFEED

WARNING DUAL POWER SOURCE SECOND SOURCE IS PHOTOVOLTAGE SYSTEM

WARNING
POWER SOURCE OUTPUT CONNECTION DO NOT RELOCATE THIS OVERCURRENT DEVICE

- ③ DC DISCONNECT/BREAKER/ RECOMBINER BOX

WARNING
ELECTRICAL SHOCK HAZARD
TERMINALS ON BOTH THE LINE AND LOAD SIDE MAY BE ENERGIZED IN THE OPEN POSITION
DO NOT WORK IS ALWAYS PRESENT WHEN SOLAR MODULES ARE EXPOSED TO SUNLIGHT

PHOTOVOLTAGE
DC DISCONNECT

PV INVERTER
DC DISCONNECT
POWER SOURCE OUTPUT
MAX SYSTEM VOLTAGE: 1000V
MAX CIRCUIT CURRENT: 40A
MAX RATED OUTPUT CURRENT OF THE CHARGE CONTROLLER OR DC-TO-DC CONVERTER (IF INSTALLED): 18A

- ④ EMT/ CONDUIT RACEWAYS

SOLAR PV DC CIRCUIT

PHOTOVOLTAGE POWER SOURCE

- ⑤ INVERTER

WARNING
THE DISCONNECTION OF THE GROUNDED CONDUCTORS MAY RESULT IN OVERVOLTAGE ON THE EQUIPMENT

AC PHOTOVOLTAGE DISCONNECT

- ⑥ PRODUCTION NET METER

WARNING DUAL POWER SOURCE SECOND SOURCE IS PHOTOVOLTAGE SYSTEM

- ⑦ AC DISCONNECT/ BREAKER/ POINTS OF CONNECTION

PHOTOVOLTAGE
AC DISCONNECT

WARNING
THIS EQUIPMENT FED BY MULTIPLE SOURCES. TOTAL RATING OF ALL OVERCURRENT DEVICES, EXCLUDING MAIN SUPPLY OVERCURRENT DEVICE, SHALL NOT EXCEED AMPACITY OF BUSBAR

- ⑧ BREAKER PANEL/ FULL BOXES

WARNING
ELECTRICAL SHOCK HAZARD
TERMINALS ON BOTH THE LINE AND LOAD SIDE MAY BE ENERGIZED IN THE OPEN POSITION

WARNING
TURN OFF PHOTO VOLTAGE AC DISCONNECT PRIOR TO WORKING INSIDE PANEL

AC PHOTOVOLTAGE DISCONNECT

AC PHOTOVOLTAGE DISCONNECT

WARNING
ELECTRICAL SHOCK HAZARD
TERMINALS ON BOTH THE LINE AND LOAD SIDE MAY BE ENERGIZED IN THE OPEN POSITION

- ⑩ BY RAPID SHUTDOWN SWITCH (WITH IN 3 FT)

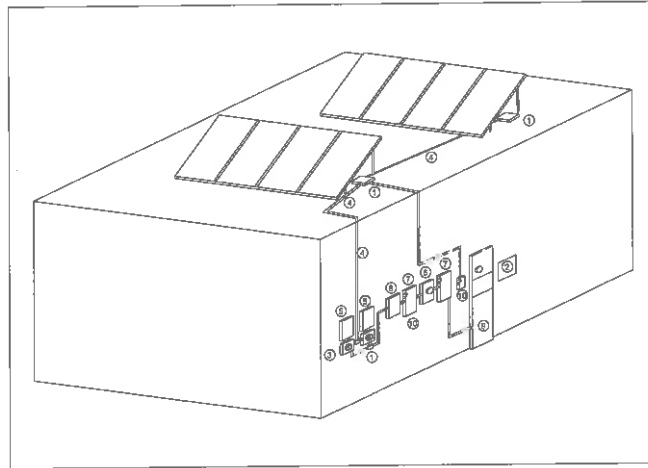
(800 941 0111) YELLOW WITH BLACK LETTERING

SOLAR PV SYSTEM EQUIPPED WITH RAPID SHUTDOWN

TURN RAPID SHUTDOWN SWITCH TO THE "OFF" POSITION TO SHUT DOWN PV SYSTEM AND REDUCE SHOCK HAZARD IN ARRAY

RAPID SHUTDOWN SWITCH FOR SOLAR PV SYSTEM

*RAPID SHUTDOWN SWITCH CAN BE EITHER THE AC DISCONNECT SWITCH OR A SEPARATE SWITCH, SEE PV 4 FOR TYPE OF RS SWITCH



SIGNAGE REQUIREMENTS

GENERAL FIRE GUIDELINES &

MARKING REQTS:

- SEC. 6 MARKING LABELS AND MARKING DEVICES
- A. PLACED PRIOR TO ELECTRICAL WORK WITH APPROPRIATE MARKING AND CLEARANCE WITH RESPECT TO REPAIRING THE SOLAR ELECTRICAL SYSTEM. THIS CAN FACILITATE IDENTIFYING ENERGIZED ELECTRICAL LINES THAT CONTACT THE SOLAR PANELS TO THE INWARD, AS THESE SHOULD NOT BE CUT WHEN WORKING FOR SMOKE REMOVAL.
- B. MAIN SERVICE DISCONNECT:
1. RESIDENTIAL BUILDINGS: THE MARKING MAY BE PLACED WITHIN THE MAIN SERVICE DISCONNECT. THE MARKING SHALL BE PLACED ON THE OUTSIDE CORNER IF THE MAIN SERVICE DISCONNECT IS OVERSIZE WITH THE SERVICE PANEL CLOSED.
 2. COMMERCIAL BUILDINGS: THE MARKING SHALL BE PLACED ADJACENT TO THE MAIN SERVICE DISCONNECT CLEARLY VISIBLE FROM THE LOCATION WHERE THE LEVER IS OPERATED.
- C. MARKINGS: SCHEMATIC, FORMAT, AND TYPE OF MATERIAL:
- A. VERBIAGE:
 - (1) LISTEN TO SOLAR ELECTRICAL SYSTEM CONNECTED
 - (2) MINIMUM 3/8" INCHES LETTER HEIGHT
 - (3) ALL LETTERS SHALL BE CAPITALIZED
 - (4) AREA OR SIMILAR FONT, HIGH-READ.
 - B. FORMAT:
 - (1) WRITE LETTERING ON A RED BACKGROUND
 - (2) MINIMUM 3/8" INCHES LETTER HEIGHT
 - (3) ALL LETTERS SHALL BE CAPITALIZED
 - (4) AREA OR SIMILAR FONT, HIGH-READ.
 - C. MATERIAL:
 - (1) REFLECTIVE, WEATHER RESISTANT MATERIAL, SUITABLE FOR THE ENVIRONMENT (USE UL -- 659 AS STANDARD FOR WEATHER RATING). DURABLE ADHESIVE MATERIALS MEET THIS REQUIREMENT.
- E. MARKING REQUIREMENTS ON DC CONDUIT, RACEWAYS, ENCLOSURES, CABLE RESEMBLES, DC CONDUITS, AND JUNCTION BOXES:
1. MARKINGS: PLACEMENT, VERBIAGE, FORMAT, AND TYPE OF MATERIAL:
 - (1) PLACED: MARKINGS SHALL BE PLACED EVERY 10 FEET ON ALL INTERIOR AND EXTERIOR DC CONDUITS, RACEWAYS, ENCLOSURES, AND CABLE ASSEMBLIES, AT TIMES, ABOVE AND FOR BELOW FLOOR/POURTHROUGHS, ALL DC CONDUITS, AND JUNCTION BOXES.
 2. VERBIAGE:
 - (1) CAUTION, SOLAR CIRCUIT
 - (2) NOTE: THE FORMAT AND TYPE OF MATERIAL SHALL ADHERE TO "PV-36" C7 OF THIS REQUIREMENT.
 - (3) INVERTERS ARE NOT REQUIRED TO HAVE CALLION MARKINGS.

MATERIALS USED FOR MARKING SHALL BE REFLECTIVE, WEATHER RESISTANT, AND SUITABLE FOR THE ENVIRONMENT. ALL LABELS SHALL BE WHITE LETTERS ON RED BACKGROUND.

THE MARKINGS SHALL BE OF SUFFICIENT DURABILITY TO WITHSTAND THE ENVIRONMENT INVOLVED.
NEC 110.21



CONTRACTOR

SUNWORKS
1030 WINDING CREEK RD. STE. 100
ROSEVILLE, CA 95678
CSLB: #441690 / C-10, C-46
(916) 409-6900

I HEREBY CERTIFY THAT THE WORK PROPOSED TO BE DONE ON THESE PLANS IS IN CONFORMANCE WITH ALL CODES AND ORDINANCES OF THE CITY OF PALMDALE AND FURTHER, BY CONTRACTING OR PROVIDING AND CONSENTING TO UNDERTAKE THAT THE WORK PERFORMED WILL BE REQUIRED TO COMPLY WITH THE CODES AND ORDINANCES OF THE CITY OF PALMDALE PRIOR TO SHALL BUILDING INSPECTION.

SIGNATURE _____ DATE _____ STATE LICENSE NO. _____
#441690 / C-10, C-46

Property Owned & Managed by:

MVRP PV
14114 BUSINESS CENTER DR.
MORENO VALLEY, CA 92553

ARCH D (2) 24" X 36" PAPER SIZE

NO.	DATE	REVISIONS	ELECT.	STRUC.
1	1/25/2025	PLANNING	ALL	-
2	01/27/2025	REVISION 1	ALL	-
3	01/27/2025	REVISION 2	ALL	-
4	01/24/2025	REVISION 3	ALL	-
5	1/22/2025	REVISION 4	ALL	-

SYSTEM INFO:

TOTAL SYSTEM SIZE: DC SPEC: 100 KW
TOTAL SYSTEM SIZE: AC SPEC: 94.8 KW

(2) 24" X 36" PAPER (2) 12" X 18" X 25" (1) SOLARWORKS SIGNAGE

DESCRIPTION:

SIGNAGE

PV 6



Q.PEAK DUO L-GB.2
415-430

- Q CELLS TECHNOLOGY LOW LEVELLED COST OF ELECTRICITY (LCOE) ALONG WITH A 25% EFFICIENCY RATIO
- REMOVES ALL RESISTIVE TECHNOLOGY LOSS BY 30% COMPARED TO CONVENTIONAL INVERTERS
- ENJOYS HIGH PERFORMANCE LONG LIFE (25 YEARS) WITH 0% DEGRADATION PER YEAR
- EXTENSIVE WEATHER RATING (IP67) FOR USE IN ALL WEATHER CONDITIONS
- 3 PHASE INVERTER (300V AC) WITH 200V DC INPUT
- STATE OF THE ART INVERTER TECHNOLOGY (MAXIMUM EFFICIENCY) WITH 99.9% EFFICIENCY

THE IDEAL SOLUTION FOR
RESIDENTIAL AND COMMERCIAL PV SYSTEMS

Three Phase Inverter with Synergy Technology

for the 277/480V Grid for North America

SE66.6KUS / SE100KUS



- Specifically designed to work with power optimizers
- Easy two-person installation - each unit mounted separately, supported with cables for secure mounting on inverter rack
- Balance of System and labor reduction compared to using multiple smaller string inverters
- Independent operation of each unit enables higher uptime and easy maintainability
- No neutral ground lead required, or hazardous voltage under the cover of the DC inclination
- Integrated arc fault protection and rapid shutdown for NEC 2017 and 2021, UL 9811, and 690.12
- Built-in modularized monitoring with fiber optic or GSM
- Feed voltage limiter for superior efficiency (99.9%) and longer strings
- Integrated DC Safety Switch
- Built-in SPD Surge Protection to better withstand lightning events

INVERTERS

Power Optimizer For North America

P860



- 25% more energy production
- Specialty designer to work with SolarEdge inverters
- Up to 25% more energy
- Superior efficiency (99.9%)
- Balance of System cost reduction (50% less cable, faster and simpler installation, over 2' longer string lengths possible)
- Fast installation with a single bolt
- Advanced maintenance with module level monitoring
- Modular design, voltage shutdown for install and firefighter safety
- Meets NEC requirements for arc fault protection (AFCE and Phase, other, Rapid Shutdown System (PRSD))
- Use with any PV modules connected in parallel

POWER OPTIMIZER



MECHANICAL SPECIFICATION

Model	SE66.6KUS / SE100KUS
Weight	15.5 kg (34.2 lbs)
Dimensions (H x W x D)	110 x 200 x 100 mm
Mounting	Standard rack mount
Temperature range	-40°C to 60°C
Humidity	5% to 95% RH
IP Rating	IP67

ELECTRICAL CHARACTERISTICS

Parameter	Value
DC Input Voltage (V _{DC})	200V
DC Input Current (I _{DC})	300A
AC Output Voltage (V _{AC})	277V / 480V
AC Output Current (I _{AC})	100A
Efficiency (%)	99.9%
MPPT Voltage Range (V _{MPPT})	150V - 400V
MPPT Current Range (A _{MPPT})	0 - 300A

INSTALLATION INFORMATION

Mounting: Standard rack mount, 19" rack compatible.

Clearance: 100mm from top, 50mm from sides.

Wiring: DC input cables, AC output cables, Fiber optic/GSM monitoring cables.

Three Phase Inverter with Synergy Technology for the 277/480V Grid for North America

SE66.6KUS / SE100KUS

Model	SE66.6KUS	SE100KUS
DC Input Voltage (V _{DC})	200V	200V
DC Input Current (I _{DC})	300A	300A
AC Output Voltage (V _{AC})	277V / 480V	277V / 480V
AC Output Current (I _{AC})	100A	100A
Efficiency (%)	99.9%	99.9%
MPPT Voltage Range (V _{MPPT})	150V - 400V	150V - 400V
MPPT Current Range (A _{MPPT})	0 - 300A	0 - 300A
Weight (kg)	15.5	15.5
Dimensions (mm)	110 x 200 x 100	110 x 200 x 100
IP Rating	IP67	IP67

Power Optimizer For North America

P860

Model	P860
DC Input Voltage (V _{DC})	200V
DC Input Current (I _{DC})	300A
AC Output Voltage (V _{AC})	277V / 480V
AC Output Current (I _{AC})	100A
Efficiency (%)	99.9%
MPPT Voltage Range (V _{MPPT})	150V - 400V
MPPT Current Range (A _{MPPT})	0 - 300A
Weight (kg)	15.5
Dimensions (mm)	110 x 200 x 100
IP Rating	IP67



CONTRACTOR

SUNWORKS
1030 WINDING CREEK RD. STE. 100
ROSEVILLE, CA 95678
CSLB: #441690 / C-10, C-46
(916) 408-6900

I HEREBY CERTIFY THAT THE WORK PROPOSED TO BE DONE ON THESE PLANS IS IN CONFORMANCE WITH ALL CODES AND ORDINANCES OF THE CITY OF FREMONT AND FURTHER, IF ORDINANCES OR ORDERS ARE DISCOVERED, I UNDERSTAND THAT THE WORK PERFORMED WILL BE REQUIRED TO COMPLY WITH THE CODES AND ORDINANCES OF THE CITY OF FREMONT PRIOR TO FINAL BUILDING INSPECTION.

SIGNATURE _____ DATE _____ STATE LICENSE NO. #441690 / C-10, C-46

Property Owned & Managed by:

MVRP PV
14114 BUSINESS CENTER DR.
MORENO VALLEY, CA 92553

ARCH D G.P.A. 1/17/17 PAPER SIZE

NO.	DATE	REVISIONS	ELECT.	STRUC.
000000	1/17/17	REVISION 1	ALL	--
010000	02/01/17	REV. DESIGN	ALL	--
020000	02/01/17	REVISION 2	ALL	--
030000	02/01/17	REVISION 4	ALL	--

SYSTEM INFO

TOTAL SYSTEM SIZE: DC STC: 102.65 KW
TOTAL SYSTEM SIZE: AC DER: 84.49 KW

2400 Q CELLS Q.PEAK DUO L-GB.2 415-430
(1) SOLAREDGE SEMIMODULE INVERTERS

EQUIPMENT SPECIFICATIONS

PV 7

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The Simple Solar Racking Solution™



FastRack510™
The Simple Solar Racking Solution™



FastRack510™

The simple solar racking solution for flat roof or ground mount PV installations.

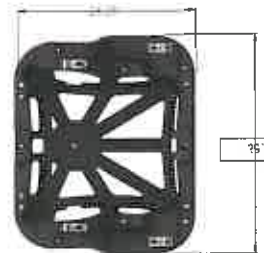
- 5° or 10° mounting solution
- Simple, modular, one piece design
- Universal design compatible with all framed modules
- Fully ballasted, heat welded, anchored and hybrid options
- Roof friendly with rounded edges and low point loads
- One size fits all with all top down connections
- Injection molded UV rated Nylon6
- Full stamp engineering and ballast layout options available
- UL 2703 Class "A" Type 1, 2, 3 Modules
- UL 437 Integrated grounding
- 100% Recyclable
- Made in the U.S.A.

Easy to install:

1 Position FastRacks

2 Add Ballast and Clamps

3 Attach Modules



	5°	10°
Row Spacing	10' (3048mm)	11' (3353mm)
Leading Edge	5' (1524mm)	6.07' (1856mm)
Module Module	41 Modules/Module (41" x 44" in width / 1027mm and 1116mm)	
Weight	4.5 lbs (2 kg)	
Ballast Requirement	4 x 6lb (1.8) Red Power (4.5 lbs each ballast) / 4074 Ballast per (2160) Modules	
Module	Specialty Module (Standard Module)	
Module Orientation	Landscape	
Wind Load Class	Miami ABC-17 to up to 175 mph	
UL Certification	UL 1703 Class "A" Type 1, 2, 3 Modules, UL 437	
Warranty	20 Year Limited Warranty (3x200H) 24.07 x 24.07 x 14	
Dimensions	61" x 13" x 25.3mm	
Disassembly	Simple disassembly and 95% recyclable content	
Paint	White (standard) / 117' Design paint (117) / 115	



CONTRACTOR

SUNWORKS
1030 WINDING CREEK RD. STE. 100
ROSEVILLE, CA 95678
CSLB: #441690 / C-10, C-46
(916) 408-6900

I HEREBY CERTIFY THAT THE WORK PROPOSED TO BE DONE ON THESE PLANS IS IN CONFORMANCE WITH ALL CODES AND ORDINANCES OF THE CITY OF FRESNO AND FURTHER, IN COMPLIANCE WITH THE STATE OF CALIFORNIA, I UNDERSTAND THAT THE WORK PERFORMED WILL BE REQUIRED TO COMPLY WITH THE CODES AND ORDINANCES OF THE CITY OF FRESNO PRIOR TO FINAL BUILDING INSPECTION.

SIGNATURE _____ DATE _____ STATE LICENSE NO. #441690 / C-10, C-46

Property Owned & Managed by:

MVPR PV
14114 BUSINESS CENTER DR.
MORENO VALLEY, CA 92553

NO.	DATE	REVISIONS	ELECT.	STRUC.
1	12/05/07	REVISED	AL	-
2	10/05/08	REVISION 1	AL	-
3	10/05/08	REVISION 2	AL	-
4	10/05/08	REVISION 3	AL	-
5	10/05/08	REVISION 4	AL	-

SYSTEM INFO:
TOTAL SYSTEM SIZE: DC STD: 102.05 KW
TOTAL SYSTEM SIZE: AC STD: 84.49 KW
(24) 0.031 0.064 (30) 0.062 425W
(1) SOLARIZE SOLARUS 400000

DESCRIPTION:
RACKING DETAILS

PV 8



intertek
Tested Quality. Assured.

Address: 3880 125 St
Columbia, MD 21046
Telephone: 410 786 0216
www.intertek.com

intertek
Tested Quality. Assured.

Address: 3880 125 St
Columbia, MD 21046
Telephone: 410 786 0216
www.intertek.com

intertek
Tested Quality. Assured.

Address: 3880 125 St
Columbia, MD 21046
Telephone: 410 786 0216
www.intertek.com

Subject: ETL Evaluation of SolarEdge Products to NEC 2017 Rapid Shutdown Requirements

To, whom it may concern

This letter represents the testing results of the below listed products to the requirements contained in the following standards:

National Electric Code, 2017, Section 690.12 requirement for rapid shutdown.

UL 1741, UL 1741 CRB for rapid shutdown

The evaluation was done on the PV Rapid Shutdown System (PVRSS), and covers installations consisting of optimizers and inverters with part numbers listed below.

The testing done has verified that controlled conditions are limited to:

- Not more than 30 volts and 240 voltamperes within 30 seconds of rapid shutdown initiation outside the array.
- Not more than 60 volts and 240 voltamperes within 30 seconds of rapid shutdown initiation inside the array.

The rapid shutdown initiation is performed by either disconnecting the AC feed to the inverter, or --if the inverter DC safety switch is readily accessible -- by turning off the DC Safety switch.

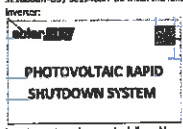
Applicable products:

- Power optimizers:

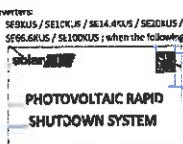
PS to lowest by 001 to 350; followed by -AGS or -TR.
OP followed by 001 to 500; followed by -IV, -MV, -IV or -EV
P followed by 001 to 850.
SP followed by 001 to 850.

INVOI

*When optimizers are connected to 2 or more modules in series, the max input voltage may exceed 60V. Following the implementation of the NEC 2017 rapid shutdown value of 60V max inside the array at the beginning of 2019, modules exceeding this combined input max voltage will be required to use optimizers with parallel inputs.



Inverter part number may be followed by a suffix



Inverter part number may be followed by a suffix

If there are any questions regarding the results contained in this report, or any of the other services offered by Intertek, please do not hesitate to contact the undersigned.

Please note, this Letter Report does not represent authorization for the use of any Intertek certification mark.

Reviewed by: Howard Liu

Title: Engineering Team Leader

Signature: [Signature]

Date: 4/21/2018

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Version: 8-September-2018

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Version: 8-September-2018

Page 3 of 3

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GFT-OP-10a

Certificate of Compliance

Certificate: 3462554

Master Contract: 254141

Project: 70167867

Date Issued: 2017-12-22

Issued to: **Heavisa O.C.E.L.A. GmbH**
17-51 Geneseeville
Interfield-Warren, Nathan-Anhalt 60706
GERMANY
Attention: Winke Engel

The products listed below are eligible to bear the CSA Mark shown with adjacent indicators 'C' and 'US' for Canada and US or with adjacent indicator 'US' for US only or without either indicator for Canada only.



Issued by: **Kenneth Cheng (Stinson) Sheng**
Kenneth Cheng (Stinson) Sheng

PRODUCES

CLASS - CSX 110 - POWER SUPPLIES-Photovoltaic Modules and Panels

CLASS - C93 190 - POWER SUPPLIES-Photovoltaic Modules and Panels - Certified to US Standards

Photovoltaic Modules with maximum system voltage: 1000V or 1500 Vdc, and Class C fire class rating and Module Efficacy Performance Type 1 or Type 2, as Stated (See Compliance Table)

Module Type	Power (Wp)	Volts (Voc)	Amps (Isc)	Temp. (°C)	
Q-STAR-GV-XXX	245	37.04	8.50	20.55	9.28
Q-MONITOR-XXX	113	37.04	8.50	20.55	9.27
Q-PRO-BIR-GV-XXX	225	37.04	8.50	20.55	9.24
Q-PRO-GV-XXX	225	37.04	8.50	20.55	9.23
MODULAR-GV-XXX	200	38.00	8.33	20.75	8.82
MODULAR-Hybrid-XXX	270	37.34	8.31	21.00	8.79
BLINE-PRO-GV-XXX	225	37.04	8.50	20.70	9.28
BLINE-PRO-BIR-GV-XXX	225	37.04	8.50	20.70	9.27
BLINE-PRO-Hybrid-XXX	270	37.04	8.50	21.00	9.29
BLINE-PRO-GV-XXX	225	37.04	8.50	20.70	9.23
Q-PT-24V-XXX	245	37.04	8.50	20.55	9.28

Certificate: 3462554
Project: 70167867

Master Contract: 254141
Date Issued: 2017-12-22

Module Type	Power (Wp)	Volts (Voc)	Amps (Isc)	Temp. (°C)	
Q-STAR-GV-XXX	245	37.04	8.50	20.55	9.28
Q-MONITOR-XXX	113	37.04	8.50	20.55	9.27
Q-PRO-BIR-GV-XXX	225	37.04	8.50	20.55	9.24
Q-PRO-GV-XXX	225	37.04	8.50	20.55	9.23
MODULAR-GV-XXX	200	38.00	8.33	20.75	8.82
MODULAR-Hybrid-XXX	270	37.34	8.31	21.00	8.79
BLINE-PRO-GV-XXX	225	37.04	8.50	20.70	9.28
BLINE-PRO-BIR-GV-XXX	225	37.04	8.50	20.70	9.27
BLINE-PRO-Hybrid-XXX	270	37.04	8.50	21.00	9.29
BLINE-PRO-GV-XXX	225	37.04	8.50	20.70	9.23
Q-PT-24V-XXX	245	37.04	8.50	20.55	9.28

Test Verification of Conformity

In the body of the certificate, the applicant of the labor product has been found to comply with the requirements of the referenced specifications at the time the tests were carried out.

Applicant Name & Address

To: **Heavisa O.C.E.L.A. GmbH**
17-51 Geneseeville
Interfield-Warren, Nathan-Anhalt 60706
GERMANY

Product Description

Photovoltaic Modules

Model

Q-STAR-GV-XXX

Manufacturer Name

Heavisa O.C.E.L.A. GmbH

Manufacturer Name Office

Heavisa O.C.E.L.A. GmbH

Date of Test

04/21/2018

Signature

[Signature]

Date

04/21/2018

sunworks
Solar Power

CONTRACTOR

SUNWORKS
1030 WINDING CREEK RD. STE. 100
ROSEVILLE, CA 95678
CSLB: #441890 / C-10, C-46
(916) 409-8900

I HEREBY CERTIFY THAT THE WORK PROPOSED TO BE DONE ON THESE PLANS IS IN CONFORMANCE WITH ALL CODES AND ORDINANCES OF THE CITY OF FRESNO AND FURTHER, IF ORDERED OR OTHERWISE DETERMINED, UNDERSTANDS THAT THE WORK PERFORMED SHALL BE REQUIRED TO COMPLY WITH THE CODES AND ORDINANCES OF THE CITY OF FRESNO PRIOR TO FINAL BUILDING INSPECTION.

SIGNATURE: [Signature] DATE: [Date] STATE LICENSE NO.: #441890-C-10, C-46

Property Owned & Managed by:

MVRP PV
14114 BUSINESS CENTER DR.
MORENO VALLEY, CA 92553

NO.	DATE	REVISIONS	ELECT.	STRUCT.
01	04/22/18	ISSUE	ALL	
02	04/22/18	REVISION 1	ALL	
03	04/22/18	REVISION 2	ALL	
04	04/22/18	REVISION 3	ALL	
05	04/22/18	REVISION 4	ALL	

SYSTEM INFO:

TOTAL SYSTEM SIZE: 36 STC, 12245 Wp

TOTAL SYSTEM SIZE: AC: 120, 8449 Wp

(24) Q-STAR-GV-XXX 1000V-600W

(1) BLINE-PRO-Hybrid-270W

DESCRIPTION:

UL LISTING

PV 9

Racking Layout



Structural analysis of the existing building structure to verify the loads imposed by the solar array system.

ARRAY 1

ARRAY 2

ARRAY 5

Condition	Minimum Separation	(ft)
Between separate solar arrays of similar construction	1.00 ft	(ft)
Between a solar array and a fixed object on the roof, or solar array of different construction	3.00 ft	(ft)
Between a solar array and a roof edge with a qualifying parapet	3.00 ft	(ft)
Between a solar array and a roof edge without a qualifying parapet	4.00 ft	(ft)

NOTES:
 1) A parapet is considered "qualifying" if the top of the parapet is not less than 6 inches above the center of mass of the solar array, and also not less than 24 inches above the adjacent roof surface.



PROJECT SPECIFICATIONS	
Roof Height (ft)	35
Wind Speed ASCE 7-10 (MPH)	95
Exposure Factor	0
Roof Slope (deg.)	191.88
Module Brand	SPRINT 160H
Module Count (PV)	372
Module Wattage (W)	430
Module Length (ft)	61.25
Module Width (ft)	40.30
Module Thickness (ft)	1.37
Module Weight (lb)	26.19
System Size (kW)	160.98
Total Panels (PV)	374
Roof Area (ft ²)	8
Roof Spacing (ft)	62.50
TR Height (ft)	10
Total CRW Blocks (PV)	279
Total Non-Block Weight (lb)	74.87
Total Block Weight (lb)	9,786
Total System Weight (lb)	23,000
Total Load / Array Area (lb/ft ²)	3,000
Site Count: 47 (FOR RETURN USE, ONLY)	
Location	14114 Redwood Center Dr., Alhambra, CA 91803
Installer	Sollega
Contact	Joseph Tarr
Email	Joe@sollegacorp.com
Phone	916.2.62.1787

NOTES:

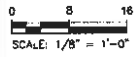
- 1. All work shall be in accordance with the approved plans.
- 2. The contractor shall be responsible for obtaining all necessary permits.
- 3. The contractor shall be responsible for the safety of all workers and the public.

David J. Sillia
 PE/SEAL/STAMP

DAVID J. SILLIA
 PE/SEAL/STAMP

Sollega
 Sollega West: 415-448-1090
 2462 Mission Dr., Ste 1070, San Francisco, CA 94110
 www.sollegacorp.com

FINAL LAYOUT: This layout was designed and approved by Sollega and other consultants. The layout was based on the available information that was provided to the designer. Sollega does not bear any responsibility for any issues that may arise from or related to the design. All drawings are a result of the work of the design team. Additional work by other parties may result in the design of the final layout. The responsibility of the designer is to ensure that the final layout is in accordance with the design team's intent. Do not copy or distribute.



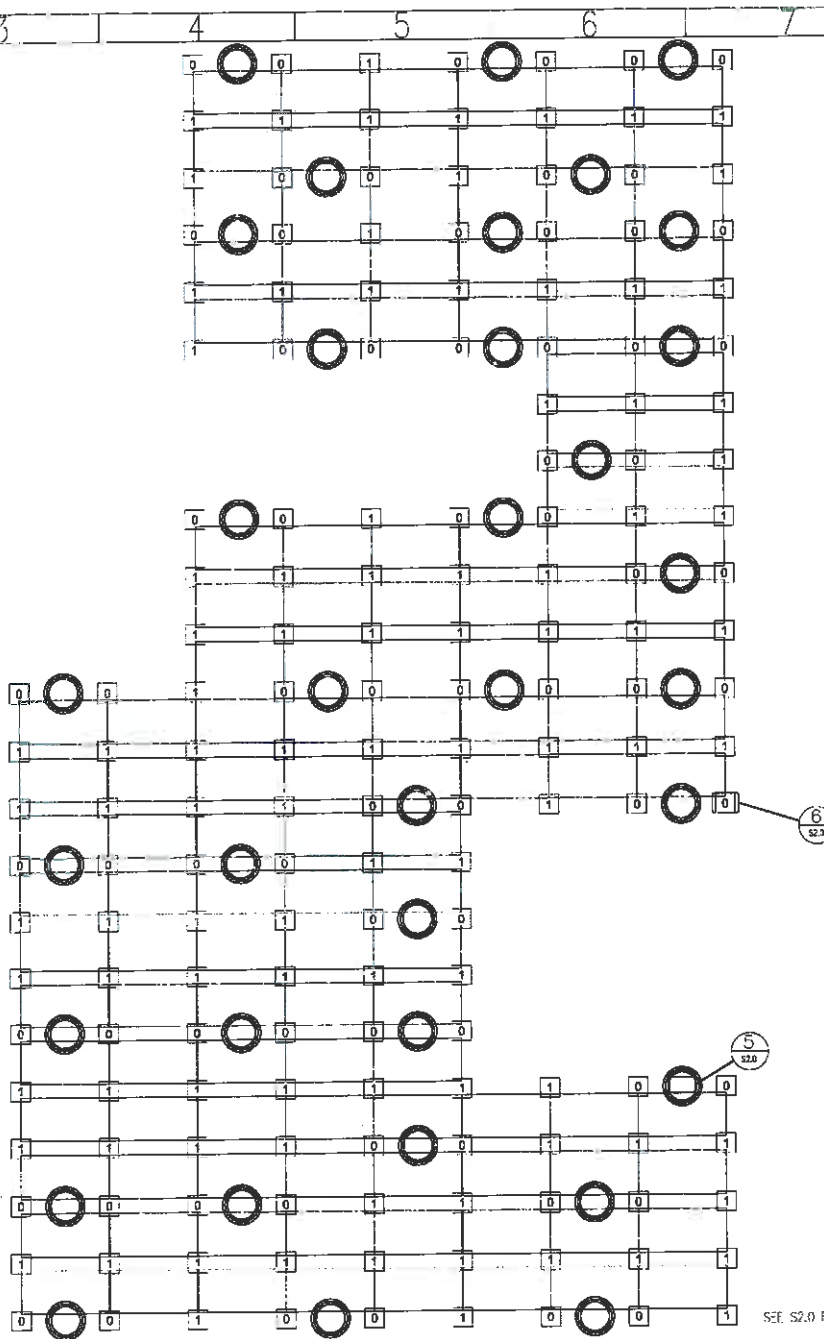
SEE S210 FOR RAILING AND/OR FENCE, AND CORNER SUPPORT ATTACHMENT DETAILS

Sheet Name	S1.0
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Ballast Layout



Seal covers analysis of the existing building structure to carry the loads imposed by the solar arrays shown here.



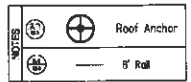
ARRAY 2

SCALE: NTS

SEE S2.0 FOR SOLLEGA ANCHOR, PA-FL, AND CORNER SUPPORT ATTACHMENT DETAILS



PROJECT SPECIFIC DATA	
Roof Height (ft)	23
Wind Speed ASCE 7-10 (MPH)	85
Expansion Factor	0
Substrate (Inch)	107.24
Module Brand	SPRINKLER
Module Count (#)	12,000
Module Weight (lb)	400
Module Length (in)	51.00
Module Width (in)	40.00
Module Thickness (in)	1.50
Module Weight (lb)	65.10
System Size (kW-DC)	194.00
Total Fasteners (in)	330
Roof Anchors (#)	20
Rail Spacing (ft)	62.00
TIE Angle (DEG)	10
Total CMU Blocks (#)	170
Total Item-Install Weight (lb)	14,070
Total Ballast Weight (lb)	4,800
Total System Weight (lb)	23,000
Total Load / Array Area (psf)	2,300
Row Count: 42 (of 48) (MIN. 15% CR. *)	
Location	16116 Redwood Center
City	San Jose, CA 95130
County	San Jose
Contract	Project 100
Client	Client Name
Phone	925.945.8197



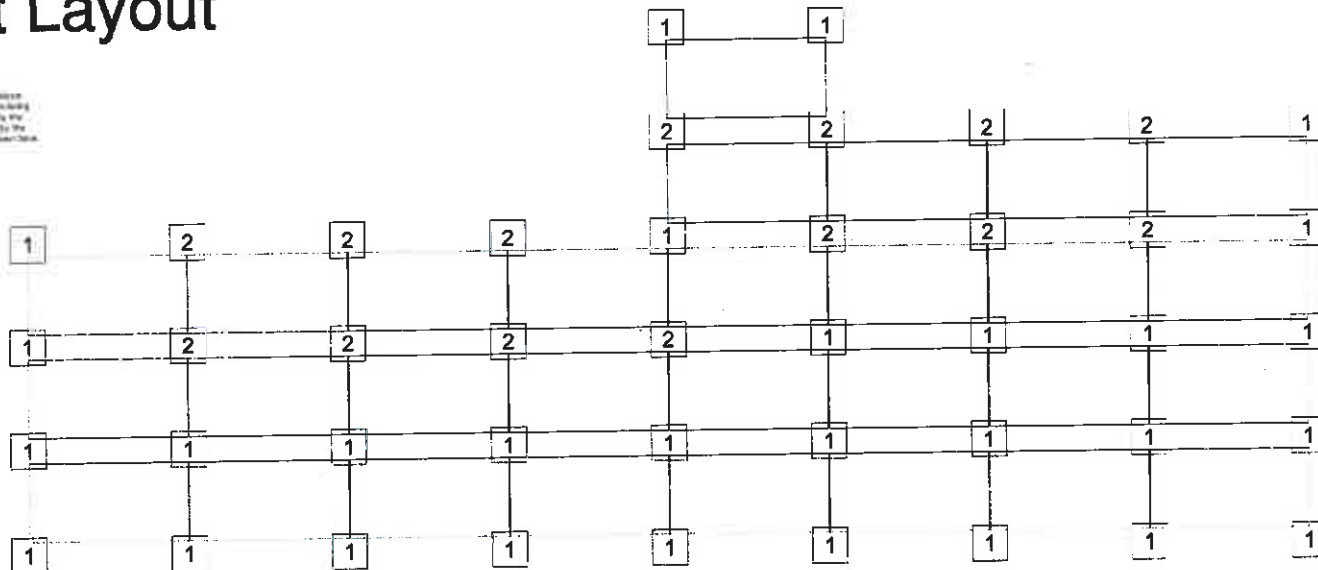
Ballast Required (#CMU blocks)
FASTRACK 510
 # = Ballast Per Fastrack
 CMU: 32.5 lb, 4"x8"x16"

Sollega
 Sollega West: 415-648-1200
 2450 Mission St., Ste. 1070, San Francisco, CA 94110
 www.Sollega.com

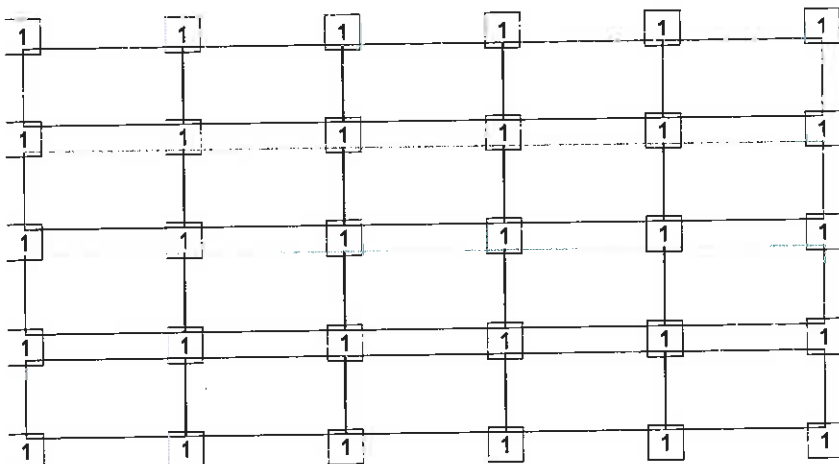
THIS LAYOUT is the property of Sollega West and is provided by Sollega West under contract. The user shall be held liable for any errors or omissions. Sollega West is not responsible for any errors or omissions. Sollega West is not responsible for any errors or omissions. Sollega West is not responsible for any errors or omissions.



Ballast Layout

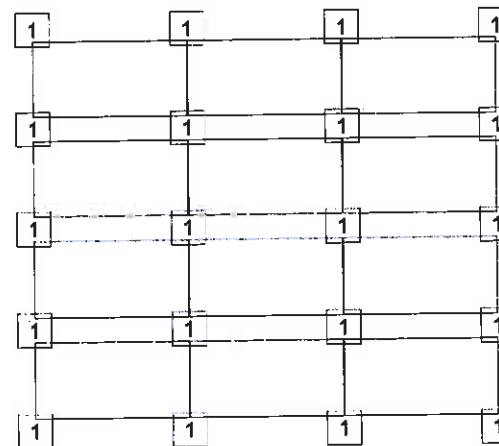


ARRAY 3



ARRAY 4

SCALE: N'S



ARRAY 5

SEE S2.0 FOR PANEL ATTACHMENT DETAILS



PROJECT SPECIFICATIONS	
Roof Height (ft)	25
Wind Speed ASCE 7-10 (MPH)	90
Seismic Factor	0
Roofpitch (deg)	187.84
Module Brand	Q CELLS 600
Module Model	L-60L2
Module Count (M)	342
Module Voltage (V)	630
Module Length (in)	61.80
Module Width (in)	49.80
Module Thickness (in)	1.33
Module Weight (lbs)	38.10
System Size (kW-DC)	194.86
Total Fasteners (M)	336
Roof Anchor (M)	21
Rail Spacing (in)	62.80
Tilt Angle (Deg)	18
Total CRW Blocks (M)	279
Total Non-Ballast Weight (lbs)	14,371
Total Ballast Weight (lbs)	9,208
Total System Weight (lbs)	23,579
Total Load / Array Area (psf)	2,300

PROJECT INFORMATION	
Location	14314 Redwood Center
City/County/State	San Jose, Santa Clara, CA 95131
Installer	Powertrak
Installer	Joseph Tran
Email	Power@powertrak.com
Phone	925.432.9187



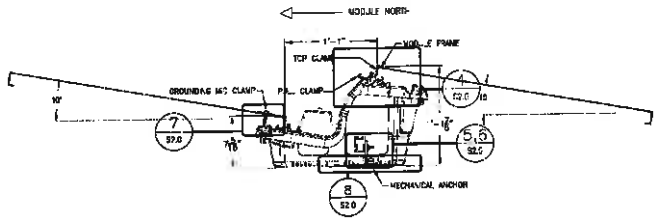
WARRANTY: This layout was designed and engineered by Sollega staff under contract. The client remains liable for the final design and construction. Sollega does not take any responsibility for any issues due to improper or incorrect installation. All dimensions should be verified in the field before installation. Field crew and installers, before or after completion of the project, if any changes are made to the original drawing, it is the responsibility of the field crew to inform the client. Sollega is not liable for any issues. No liability. Do not copy or alter base.



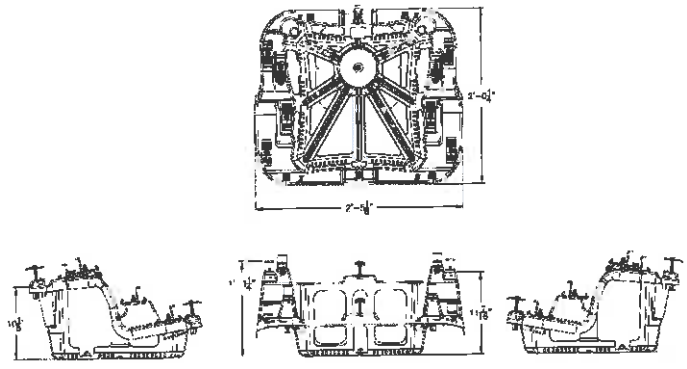


Steel anchors attached to the existing building structure to locate the loads imposed by the solar array about 500 lbs.

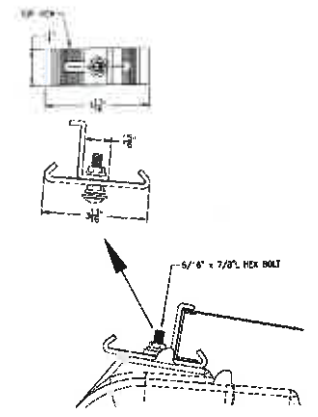
10/1/2009, 8:30 AM



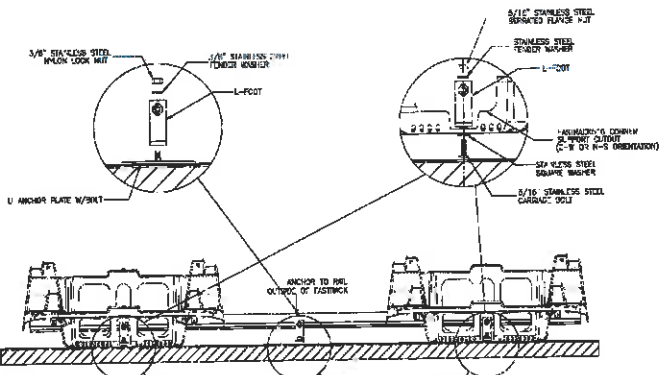
1 MODULE ATTACHMENT DETAIL
SCALE: 1/16" = 1'-0"



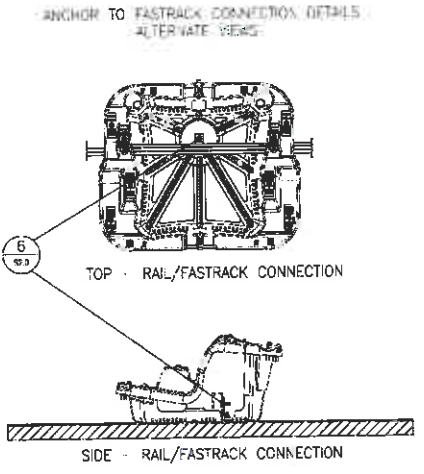
3 SO_EGA FAST RACK 510 (FR510)
SCALE: 1-1/2" = 1'-0"



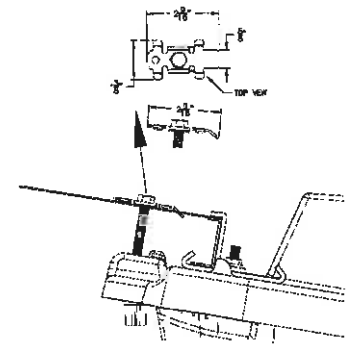
4 CLAMP DETAIL
SCALE: 6" = 1'-0"



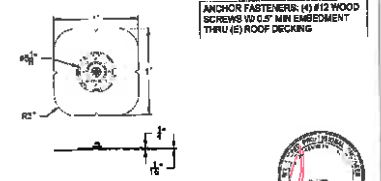
5 ANCHOR / RAIL / FASTRACK CONNECTION DETAILS
SCALE: 1/8" = 1'-0"



6 ANCHOR TO FASTRACK CONNECTION DETAILS - ALTERNATE VIEWS
SCALE: 1/8" = 1'-0"



7 MID CLAMP DETAIL
SCALE: 6" = 1'-0"



8 MECHANICAL ANCHOR DETAIL
SCALE: 1-1/2" = 1'-0"



Sollega: 415-648-1289
2480 Mission Street, Ste. 107B, San Francisco, CA 94110
www.Sollega.com

NOTES

Location	14114 Industrial Center So. Ramon Valley, CA 94523
Client	Summit
Contact	Joseph Yee
Email	JoeY@summit.com
Phone	925.263.8187

DRAWING INFO.	
Date & Time	8/9/2019 2:55 PM
Prepared/Approved by	JC / R
Sheet Name	SZ.0

NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893**. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The City of Moreno Valley Community Development Department should be contacted on non-ALUC issues. For more information please contact City of Moreno Valley contact Mr. Jesus Huerta at (951) 413-3234.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to prull@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please telephone Barbara Santos at (951) 955-5132.

PLACE OF HEARING: **Riverside County Administration Center**
 4080 Lemon Street, 1st Floor Board Chambers
 Riverside California

DATE OF HEARING: **May 13, 2021**

TIME OF HEARING: **9:30 A.M.**

Pursuant to Executive Order N-25-20, this meeting will be conducted by teleconference and at the Place of Hearing, as listed above. Public access to the meeting location will be allowed, but limited to comply with the Executive Order. Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org

CASE DESCRIPTION:

ZAP1458MA21 – MVRP PV (Representative: Sunworks Inc.) – City of Moreno Valley Case No. PEN20-0198 (Plot Plan), a proposal to construct a 5,588 square foot rooftop solar panel system on an existing industrial building on 1.62 acres located at 14114 Business Center Drive, northerly of Golden Crest Drive, westerly of Elsworth Street, and southerly of Alessandro Boulevard (Airport Compatibility Zone C1 of the March Air Reserve Base/Inland Port Airport Influence Area).



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP1458MA21 DATE SUBMITTED: 3-4-21

APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

Applicant	Sunworks Inc.	Phone Number	(916) 409-6900
Mailing Address	1030 Winding Creek Road #100 Roseville, CA 95678	Email	jtarr@sunworksusa.com

Representative	Alexander Martinez	Phone Number	626-716-2916
Mailing Address	990 S. Arroyo Parkway #4 Pasadena, CA 91105	Email	projectmanager@cfa-la.com

Property Owner	MVRP PV	Phone Number	()
Mailing Address	14114 Business Center Drive Moreno Valley, CA 92553	Email	

LOCAL JURISDICTION AGENCY

Local Agency Name	Moreno Valley Community Development	Phone Number	(951) 413-3234
Staff Contact	Jesus Huerta	Email	jesush@moval.org
Mailing Address	14177 Frederick Street Moreno Valley, CA 92553	Case Type	<input checked="" type="checkbox"/> General Plan / Specific Plan Amendment <input type="checkbox"/> Zoning Ordinance Amendment <input type="checkbox"/> Subdivision Parcel Map / Tentative Tract <input checked="" type="checkbox"/> Use Permit <input checked="" type="checkbox"/> Site Plan Review/Plot Plan <input type="checkbox"/> Other
Local Agency Project No	PEN20-0198		

March
CI

PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address	14114 Business Center drive Moreno Valley, CA 92553	Gross Parcel Size	1.62 acres
Assessor's Parcel No.	297-210-026	Nearest Airport and distance from Airport	MarchAFB 1700
Subdivision Name	POM 21626		
Lot Number	6		

PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe)	The existing land use is a 18,240 sqft existing office space with an existing parking Please see attached
------------------------------	--

Proposed Land Use (describe)	Please see attached The proposed scope of work is to add solar pannels to the existing building.	
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units)	N/A
For Other Land Uses (See Appendix C)	Hours of Operation	N/A
	Number of People on Site	Maximum Number 300
	Method of Calculation	Based on C of O and occupancies for existing office building
Height Data	Site Elevation (above mean sea level)	1561 ft.
	Height of buildings or structures (from the ground)	30 ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
	If yes, describe	According to glare study the installation of proposed solar pannels produce any "green" or "yellow" glare. Please see attached

- A. NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:**
1. Completed ALUC Application Form
 1. ALUC fee payment
 1. Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
 1. Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
 1. CD with digital files of the plans (pdf)
 1. Vicinity Map (8.5x11)
 1. Detailed project description
 1. Local jurisdiction project transmittal
 3. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
 3. Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. (Only required if the project is scheduled for a public hearing Commission meeting)

**COUNTY OF RIVERSIDE
AIRPORT LAND USE COMMISSION**

STAFF REPORT

AGENDA ITEM: 3.5

HEARING DATE: May 13, 2021

CASE NUMBER: ZAP1462MA21 – Nautilus Abrasives Blasting and Powder Coating, LLC (Representative: Renova Energy)

APPROVING JURISDICTION: City of Moreno Valley

JURISDICTION CASE NO: BAP21-0330 (Building Permit)

LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan

Airport Influence Area: March Air Reserve Base

Land Use Policy: Zones B1-APZ-I and B2

Noise Levels: 65 – 70 CNEL contour

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends that the Commission CONTINUE the matter to the June 10, 2021 meeting, pending completion of the Air Force review of the project.

PROJECT DESCRIPTION: A proposal to construct a 7,347 square foot rooftop solar panel system on an existing industrial building on 1.0 acre.

PROJECT LOCATION: The site is at 14451 Commerce Center Drive, northerly of Cactus Avenue, easterly of Old 215 Frontage Road, and southerly of Alessandro Boulevard, in the City of Moreno Valley, approximately 4,350 feet northwesterly of the northerly end of Runway 14-32 at March Air Reserve Base.

BACKGROUND:

Non-Residential Land Use Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zones B1-APZ-I and B2, which limits average intensity to 25 people per acre and 100 people per single acre in Zone B1-APZ-I, and limits average intensity to 100 people per acre and 250 people per single acre in Zone B2. The proposed rooftop solar panels will not generate any occupancy.

March Air Reserve Base/United States Air Force Input: Given that the project site is located in Zones B1-APZ-I and B2 northwesterly of the northerly runway at March Air Reserve Base, the March Air Reserve Base staff was notified of the proposal to add rooftop solar panels, and sent a solar glare hazard analysis study for their review. As of the time this staff report was prepared, no comments have been received from the Air Force regarding this project.

Flight Hazard Issues: Structure height, electrical interference, and reflectivity/glare are among the issues that solar panels in the airport influence area must address. The project's 7,347 square foot square foot photovoltaic (PV) panel structures would be located on the rooftop of the existing industrial building within Compatibility Zones B1-APZ-I and B2.

Glint and Glare/Reflectivity

Based on the Federal Aviation Administration's Interim Policy for Review of Solar Energy System Projects on Federally Obligated Airports, no glare potential or low potential for temporary after-image ("green" level) are acceptable levels of glare on final approach (within 2 miles from end of runway) for solar facilities located on airport property. However, potential for temporary after-image" ("yellow" level) and potential for permanent eye damage ("red" level) are not acceptable levels of glare on final approach. No glare is permitted at air traffic control towers.

The project proposes 7,347 square feet of solar panels on the existing building rooftop with a fixed tilt of 5 degrees with no rotation, and an orientation of 180 degrees. The applicant has submitted a glare analysis utilizing the web-based Forge Solar, a copy of which is attached hereto. The analysis was based on a 2 mile straight in approach (as per FAA Interim Policy standards) to runways 14 and 32, and also based on the traffic patterns as identified by March Air Reserve Base staff (Runway 12/30 General Aviation, Runway 14/32 General Aviation, Runway 14/32 C-17/KC-135, Runway 14/32 Overhead). The analysis utilized a glide slope approach of 5.0 degrees. No glare would affect the Air Traffic Control Tower.

The analysis concluded that no glare would occur on the 2 mile approach to runways 14 and 32. However, some potential for glare was identified within the Air Force traffic pattern. Evaluation of the Air Force traffic patterns indicates that the panels would result in low potential for temporary after-image ("green" level glare). All times are in standard time.

Runway 14/32 C-17/KC-135 traffic pattern:

- Runway 32 Crosswind portion, totaling 36 minutes of "green" level glare, lasting up to 3 minutes a day, in February to March, and October to November from 6:00 a.m. to 7:00 a.m.
- Runway 32 Upwind portion, totaling 2,456 minutes of "green" level glare, lasting up to 15 minutes a day, between September to April from 7:00 a.m. to 8:00 a.m.

Runway 12/30 General Aviation traffic pattern:

- Runway 30 Crosswind portion, totaling 310 minutes of "green" level glare, lasting up to 10 minutes a day, in April to May, and August to September from 4:00 p.m. to 5:00 p.m.

- Runway 30 Downwind portion, totaling 1,888 minutes of “green” level glare, lasting up to 15 minutes a day, between April to October from 5:00 p.m. to 6:00 p.m.

Runway 14/32 General Aviation traffic pattern:

- Runway 32 Crosswind portion, totaling 2,942 minutes of “green” level glare, lasting up to 50 minutes a day, between May to August, from 6:00 a.m. to 7:00 a.m.

The total of 7,632 minutes of “green” level glare represents less than 3 percent of total day light time.

Electrical and Communication Interference

The applicant has indicated that they do not plan to utilize equipment that would interfere with aircraft communications. The PV panels themselves present little risk of interfering with radar transmission due to their low profiles. In addition, solar panels do not emit electromagnetic waves over distances that could interfere with radar signal transmissions, and any electrical facilities that do carry concentrated current will be buried beneath the ground and away from any signal transmission. There are no radar transmission or receiving facilities within the site.

Prohibited and Discouraged Uses: Glare from solar panels could potentially constitute a hazard to flight. However, based on the solar glare hazard analysis provided, the glare experienced would result in a low potential for temporary after-image (“green” level) which has been determined by the Federal Aviation Administration (FAA) to be an acceptable level for solar facilities on airports. Therefore, the hazard potential is low. Staff has included conditions to remedy unanticipated situations.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site within the 65-70 CNEL range from aircraft noise. The proposed solar panels are a non-noise sensitive use, therefore no mitigation measures are necessary.

Part 77: The elevation of Runway 14-32 at its northerly terminus is 1,535 feet above mean sea level (1,535 feet AMSL). At a distance of approximately 4,350 feet from the runway to the site, Federal Aviation Administration (FAA) review would be required for any structures with top of roof elevation exceeding 1,579 feet AMSL. The site’s finished floor elevation is 1,555 feet AMSL and the existing building height is 20 feet. The establishment of the proposed rooftop solar panels would increase the maximum elevation by 1 foot, for a top point elevation of 1,576 feet AMSL. Therefore, review by the FAA Obstruction Evaluation Service is not required.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.

2. The following uses/activities are not included in the proposed project and shall be prohibited at this site.
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight or circling climb following takeoff or toward an aircraft engaged in a straight or circling final approach toward a landing at an airport, other than a DoD or FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport to the extent as to result in a potential for temporary after-image greater than the low ("green") level.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, congregate care facilities, hotels/motels, places of assembly (including, but not limited to places of worship and theaters), buildings with more than 2 aboveground habitable floors, hazardous materials and critical community infrastructure facilities.
 - (f) Highly noise-sensitive outdoor nonresidential uses. Examples of noise-sensitive outdoor nonresidential uses that are prohibited include, but are not limited to, major spectator-oriented sports stadiums, amphitheaters, concert halls and drive-in theaters.
 - (g) Any other uses not permitted in (Accident Potential Zone I/ Accident Potential Zone II) pursuant to DoDI 4165.57 Appx.2, Tbl.1.
 - (h) Hazards to Flight.
3. Prior to issuance of building permits, the landowner shall convey an avigation easement to the March Inland Port Airport Authority or its successor in interest, or provide evidence that such easement has been previously conveyed. The Airport Authority may waive this requirement in the event that the Authority determines that pre-existing avigation easements

dedicated to the United States of America are sufficient to address its needs. Contact the March Joint Powers Authority at (951) 656-7000 for additional information.

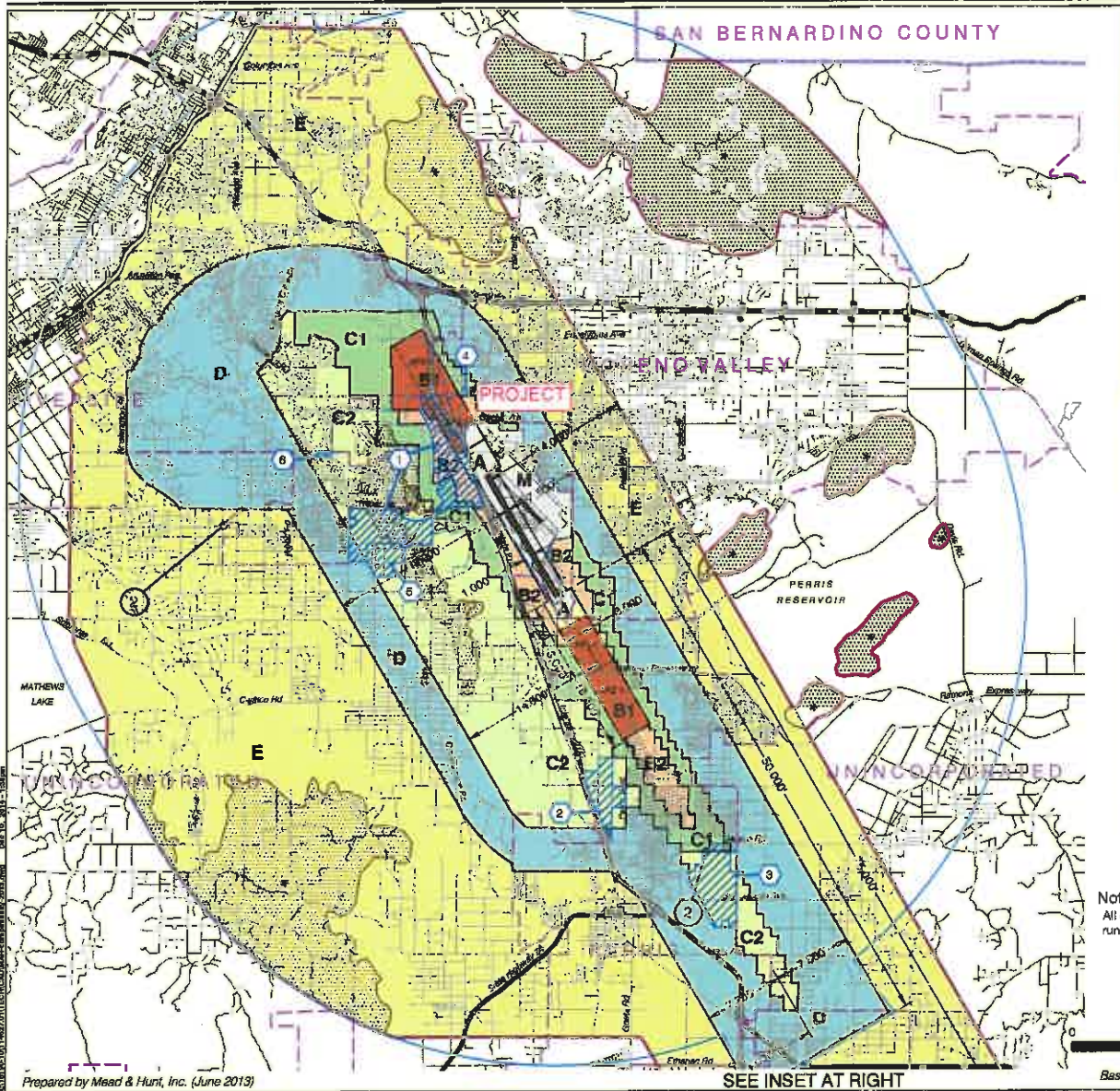
4. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
5. The maximum height of the building, including all roof-mounted equipment including solar panels, if any, shall be limited to 21 feet, and the maximum top point elevation shall not exceed 1,579 feet above mean sea level unless a "Determination of No Hazard to Air Navigation" letter authorizing a higher top point elevation has been issued by the Federal Aviation Administration Obstruction Evaluation Service.
6. All solar arrays installed on the project site shall consist of smooth glass photovoltaic solar panels without anti-reflective coating, a fixed tilt of 5 degrees and orientation of 180 degrees. Solar panels shall be limited to a total of 7,347 square feet, and the locations and coordinates shall be as specified in the glare study. Any deviation from these specifications (other than reduction in square footage of panels), including change in orientation, shall require a new solar glare analysis to ensure that the amended project does not result in any glare impacting the air traffic control tower or creation of any "yellow" or "red" level glare in the flight paths, and shall require a new hearing by the Airport Land Use Commission.
7. In the event that any glint, glare, or flash affecting the safety of air navigation occurs as a result of project operation, upon notification to the airport operator of an event, the airport operator shall notify the project operator in writing. Within 30 days of written notice, the project operator shall be required to promptly take all measures necessary to eliminate such glint, glare, or flash. An "event" includes any situation that results in an accident, incident, "near-miss," or specific safety complaint regarding an in-flight experience to the airport operator or to federal, state, or county authorities responsible for the safety of air navigation. The project operator shall work with the airport operator to prevent recurrence of the incidence. Suggested measures may include, but are not limited to, changing the orientation and/or tilt of the source, covering the source at the time of day when events of glare occur, or wholly removing the source to diminish or eliminate the source of the glint, glare, or flash. For each such event made known to the project operator, the necessary remediation shall only be considered to have been fulfilled when the airport operator states in writing that the situation has been remediated to the airport operator's satisfaction.
8. In the event that any electrical interference affecting the safety of air navigation occurs as a result of project operation, upon notification to the airport operator of an event, the airport operator shall notify the project operator in writing. Within 30 days of written notice, the project operator shall be required to promptly take all measures necessary to eliminate such interference. An "event" includes any situation that results in an accident, incident, "near-miss," report by airport personnel, or specific safety complaint to the airport operator or to

federal, state, or county authorities responsible for the safety of air navigation. The project operator shall work with the airport operator to prevent recurrence of the event. For each such event made known to the project operator, the necessary remediation shall only be considered to have been fulfilled when the airport operator states in writing that the situation has been remediated to the airport operator's satisfaction.

Y:\AIRPORT CASE FILES\March\ZAP1462MA21\ZAP1462MA21sr.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



LEGEND

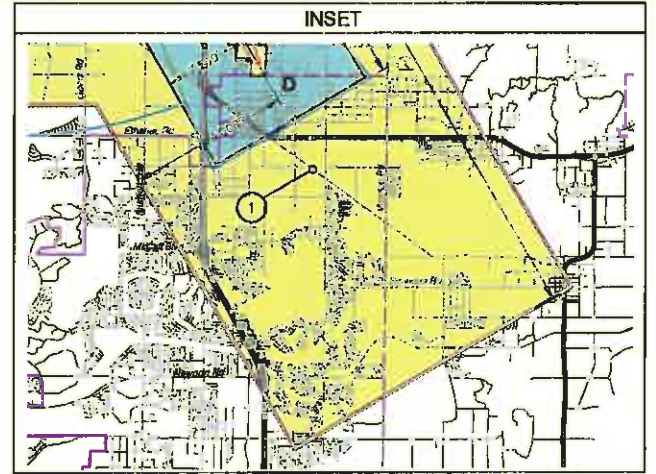
Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)
- 1 March JPA: Merch Business Center/Meridian
- 2 Perris: Harvest Landing
- 3 Perris: Park West
- 4 Moreno Valley: Affordable Housing
- 5 March JPA: Ben Clark Training Center
- 6 Riverside: Ridge Crest Subdivision

- ① Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,595 feet MSL.
- ② Point at which departing aircraft typically reach 3,000 feet above runway end.



**Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)**

Note:
All dimensions are measured from runway ends and centerlines.



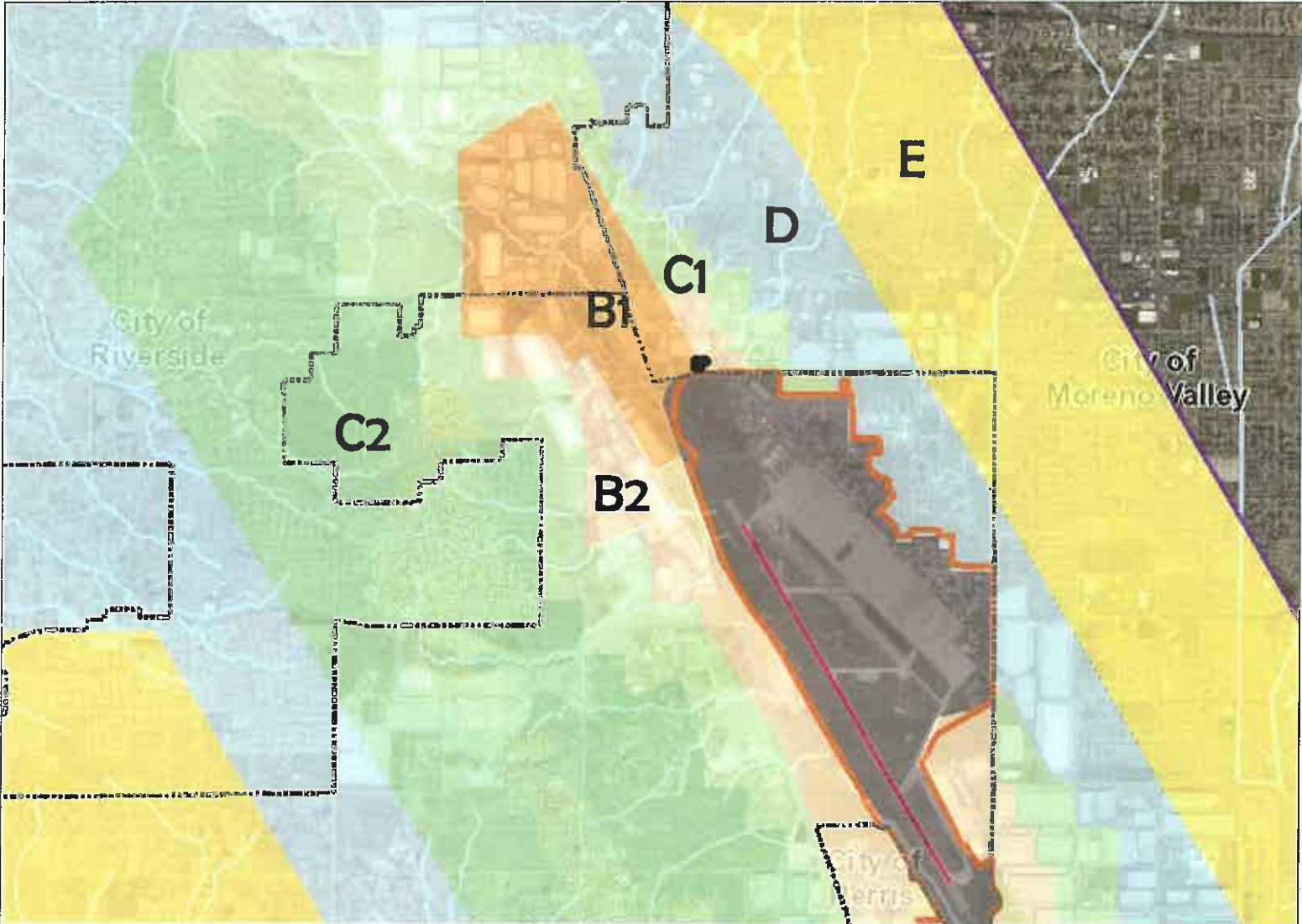
Base map source: County of Riverside 2013

Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map MA-1
Compatibility Map
March Air Reserve Base / Inland Port Airport

Map My County Map



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



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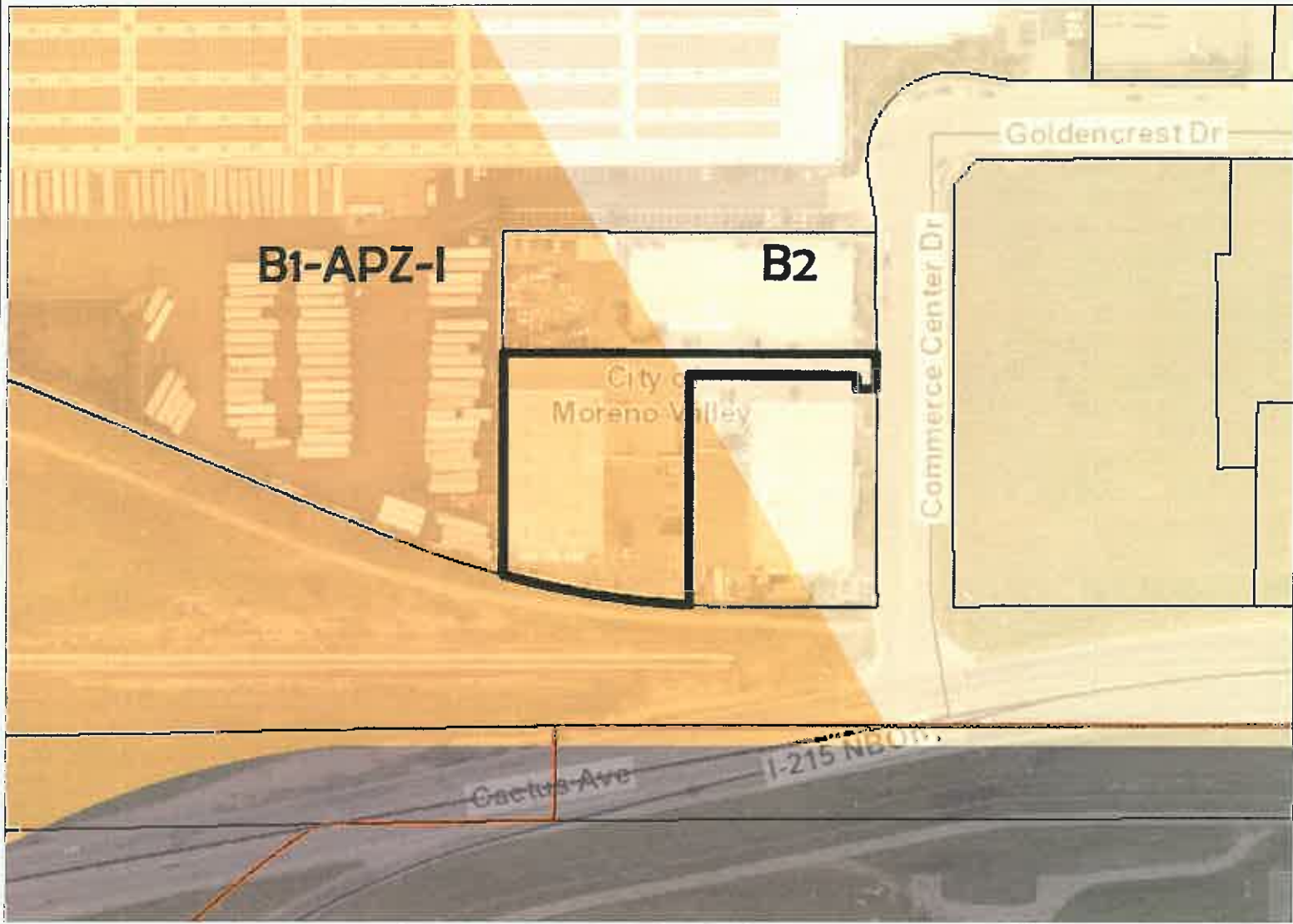
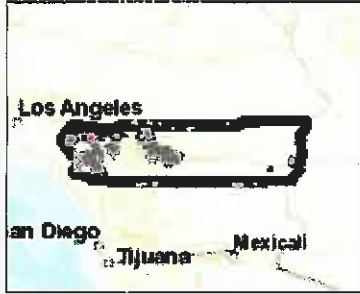


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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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Notes

Map My County Map



Legend

-  Parcels
-  County Centerline Names
-  County Centerlines
-  Blueline Streams
-  City Areas
-  World Street Map



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Notes

Map My County Map



Legend

- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



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Notes

Map My County Map



Legend

- Parcels
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



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Notes

Map My County Map



Legend

-  Parcels
-  County Centerlines
-  Blueline Streams
-  City Areas
-  World Street Map



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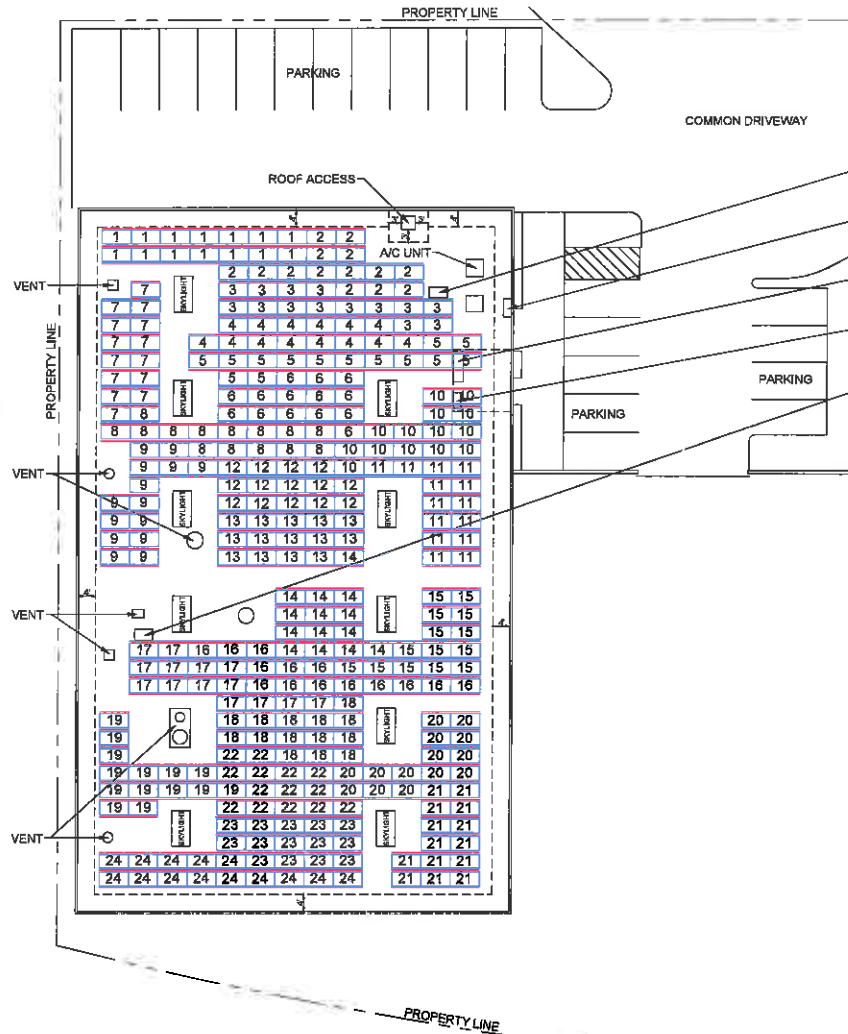
Notes

ALL CONSTRUCTION SHALL COMPLY WITH THE 2019 CALIFORNIA BUILDING CODE, 2019 ELECTRICAL CODE BASED ON THE 2017 NATIONAL ELECTRICAL CODE.

NOTES:

- THIS IS A 151,200 KW SOLAR ELECTRIC SYSTEM USING (336) SUNPOWER SPR-A450-COM-MLSD (450 WATT) PHOTOVOLTAIC MODULES
- THIS SYSTEM IS GRID-INTERIED VIA UL-LISTED POWER CONDITIONING INVERTERS:
(2) SUNNYY TRIPPOWER CORE1 62-US-41, 480Y/277 VOLT, 3Ø
- THIS SYSTEM HAS NO UPS, NO BATTERIES.
- THIS IS A ROOF MOUNTED SYSTEM, ADDING 5.83 LB/SQ FT. ROOF IS FOAM AT 0° PITCH
- THE SYSTEMS SQUARE FOOTAGE IS 7,346 FT² (44% OF TOTAL ROOF AREA). MODULES HAVE AN AZIMUTH OF 180°
- MODULES ARE 5° TILT ABOVE ROOF SLOPE, NOT TO EXCEED 16° ABOVE ROOF SURFACE.
- THIS IS A SINGLE STORY BUILDING.
- MODULES WILL BE SECURED TO ROOFTOP USING A SINGLE TILT BALLASTED HELIX SYSTEM AND OMG POWERGRIP UNIVERSAL 83 MECHANICAL ATTACHMENTS WHERE REQUIRED.
- DESIGNED FOR BASIC WIND SPEED OF 110 MPH (WIND EXPOSURE C) PER SUNPOWER CALCULATIONS, SEE SPECS.
- VISIBLE EXPOSED CONDUIT SHALL BE PAINTED TO MATCH.
- MODULE DIMENSIONS ARE 40.0" X 78.7"
- SYSTEM INSTALLATION SHALL BE PER NEC 690 AND IS SUBJECT TO FIELD INSPECTION BY THE CITY.
- PROPER CLEARANCES SHALL BE IN ACCORDANCE WITH CAL FIRE GUIDELINES.

SOLAR SYSTEM SIZE: 151,200 KW DC
CEC AC SYSTEM SIZE: 137,738 KW AC



INVERTER A	
STRING #	STRING #
1 - MPPT 1 - INPUT #1	2 - MPPT 1 - INPUT #2
3 - MPPT 2 - INPUT #1	4 - MPPT 2 - INPUT #2
5 - MPPT 3 - INPUT #1	6 - MPPT 3 - INPUT #2
7 - MPPT 4 - INPUT #1	8 - MPPT 4 - INPUT #2
9 - MPPT 5 - INPUT #1	10 - MPPT 5 - INPUT #2
11 - MPPT 6 - INPUT #1	12 - MPPT 6 - INPUT #2

INVERTER B	
STRING #	STRING #
13 - MPPT 1 - INPUT #1	14 - MPPT 1 - INPUT #2
15 - MPPT 2 - INPUT #1	16 - MPPT 2 - INPUT #2
17 - MPPT 3 - INPUT #1	18 - MPPT 3 - INPUT #2
19 - MPPT 4 - INPUT #1	20 - MPPT 4 - INPUT #2
21 - MPPT 5 - INPUT #1	22 - MPPT 5 - INPUT #2
23 - MPPT 6 - INPUT #1	24 - MPPT 6 - INPUT #2

- 62KW INVERTER (A)
SUNNYY TRIPPOWER CORE1 62-US-41
480Y/277 VOLT, 3Ø
LOCATED ON ROOFTOP
- 250 AMP AC LOAD CENTER
SIEMENS P1E19MC250AT WITH
SIEMENS WP32 ENCLOSURE
480V, 3Ø, NEMA 3R
LOCATED ON ROOFTOP
- EXISTING METER #058000-022781
EXISTING 800 AMP MAIN SWITCHGEAR
480/277 VOLT, 3Ø, 4W
LOCATED INSIDE UTILITY ROOM
- 200 AMP FUSIBLE AC DISCONNECT
SQUARE-D #1034
600V, 3Ø, NEMA 1
LOCATED NEXT TO MAIN
- 62KW INVERTER (B)
SUNNYY TRIPPOWER CORE1 62-US-41
480Y/277 VOLT, 3Ø
LOCATED ON ROOFTOP

SCOPE OF WORK:
Project consists of installing a roof mounted 151,200 kw DC commercial solar photovoltaic (PV) system comprised of:
(336) Sunpower 450 watt PV modules (Ungrounded) (#SPR-A450-COM-MLSD),
(2) STP CORE-62-US-41 Inverter, 480/277 VOLT, 3Ø
(1) 250 Amp AC Load Center
(1) 200 Amp Square-D Fusible AC Disconnect
Conduits w/fitings and w/res, Single Tilt Ballasted Helix System, and all necessary electrical equipment to provide a renewable power source for the customer.

PROJECT CONTACTS:
CONTRACTOR: RENOVA
permits@renovaenergy.com
PRIMARY CONTACT:
MATTHEW DE LA TORRE
SECONDARY CONTACT:
Jamie Silva

BUILDING ZONING: I
TYPE: INDUSTRIAL / BUSINESS PARK
WIND DESIGN SPEED: 110 MPH
WIND EXPOSURE: C
SEISMIC DESIGN CATEGORY: D
SPRINKLERED: YES

APPLICABLE CODES:
CALIFORNIA RESIDENTIAL CODE 2019
CALIFORNIA ELECTRICAL CODE 2019
NATIONAL ELECTRICAL CODE 2017
CALIFORNIA AMENDMENTS 2019
CITY OF MORENO VALLEY MUNICIPAL CODE



Project Name:
NAUTILUS ABRASIVES BLASTING AND POWDER COATING LLC (COM19799)
14451 COMMERCE CENTER DRIVE
MORENO VALLEY, CA. 92553

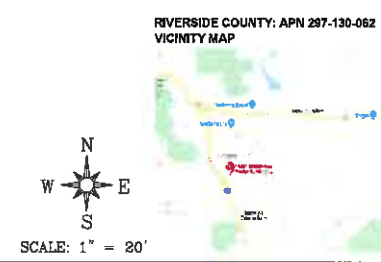
Drawn By: *DT*
DAVID THORNBURY

Date Drawn:
01/04/2021

Revision No.
0

Sheet No.

S1
SITE PLAN



AC LOAD CENTER/DISCONNECT

WARNING

ELECTRIC SHOCK HAZARD. DO NOT TOUCH TERMINALS. TERMINALS ON BOTH THE LINE AND LOAD SIDES MAY BE ENERGIZED IN THE OPEN POSITION.

MAX AC OPERATING CURRENT: 159.0 A
OPERATING AC VOLTAGE: 480 Volts

MAIN PANEL

THIS PANEL IS FED BY TWO SOURCES: UTILITY AND SOLAR PHOTOVOLTAIC

AC DISCONNECT

RAPID SHUTDOWN SWITCH FOR SOLAR PV SYSTEM

LOAD CENTER

PHOTOVOLTAIC DISCONNECT BREAKER LOCATED INSIDE

MAIN PANEL

PHOTOVOLTAIC SYSTEM EQUIPPED WITH RAPID SHUTDOWN

JUNCTION BOX

WARNING

ELECTRIC SHOCK HAZARD. THE DC CONDUCTORS OF THE PHOTOVOLTAIC SYSTEM ARE UNGROUNDED AND MAY BE ENERGIZED.

INVERTER

WARNING

ELECTRIC SHOCK HAZARD IF A GROUND FAULT IS INDICATED, THE NORMALLY GROUNDED CONDUCTORS MAY BE UNGROUNDED AND ENERGIZED.

SOLAR BREAKER

WARNING

INVERTER OUTPUT CONNECTION DO NOT RELOCATE THIS OVER CURRENT DEVICE.

DC DISCONNECT INVERTER(S)

WARNING

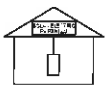
ELECTRIC SHOCK HAZARD. DO NOT TOUCH TERMINALS. TERMINALS ON BOTH THE LINE AND LOAD SIDES MAY BE ENERGIZED IN THE OPEN POSITION.

OPERATING CURRENT: 105.8 Amps
OPERATING VOLTAGE: 616.0 Volts
MAXIMUM VOLTAGE: 788.5 Volts
SHORT-CIRCUIT CURRENT: 113.4 Amps

AC DISCONNECT

PHOTOVOLTAIC SYSTEM EQUIPPED WITH RAPID SHUTDOWN

TURN RAPID SHUTDOWN SWITCH TO THE "OFF" POSITION TO SHUT DOWN PV SYSTEM AND REDUCE SHOCK HAZARD IN THE ARRAY

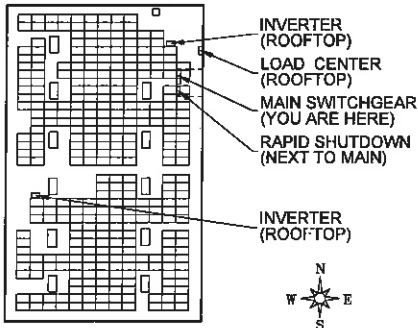


NOTE: RED BACKGROUND WITH WHITE LETTERING 3/8" LETTER HEIGHT

MAIN PANEL: MECHANICALLY ATTACHED

WARNING

THIS FACILITY IS EQUIPPED WITH A PHOTOVOLTAIC GENERATOR LOCATED ON THE ROOF AS SHOWN. THE SYSTEM MAY BE DISCONNECTED FROM THE FACILITY PER THE AC DISCONNECT AS SHOWN.



SAFEGUARDS

- SOLAR BREAKER SHALL BE LISTED FOR USE IN EXISTING MAIN PANEL. ACCEPTED BREAKER REPLACEMENTS ARE EATON, SQUARE-D & GE.
- ALL JUNCTION BOXES SHALL BE NEMA OUTDOOR/RAINTIGHT RATED.
- WILEY CABLE CLIPS SHALL BE USED.
- ALL EMT SHALL USE OUTDOOR RAIN-TIGHT COMPRESSION FITTINGS.
- ALL EQUIPMENT SHALL BE LABELED.
- ADD COMBINER / JUNCTION BOXES AS NEEDED.
- AN INTERNAL BLOCKING DIODE IS PROVIDED IN EACH MODULE.
- GROUND FAULT PROTECTION DEVICE IS INTERNALLY BUILT INTO SUNPOWER INVERTERS.
- MODULE GROUNDING SHALL BE MADE WITH ILSCO GBL-4DB LUGS AND #8 SOLID BARE COPPER USING WEEB CONNECTIONS.
- ALL NEUTRAL TO GROUND CONNECTIONS SHALL OCCUR IN THE MAIN PANEL.
- EMT SHALL BE 1" MINIMUM.
- JUNCTION BOX/ COMBINER BOX ENTRY SHALL USE COMPRESSION FITTINGS.
- ALL WIRING BETWEEN AC MODULES SHALL BE PV-WIRE AND AC CABLE.
- ALL CONDUIT SHALL BE 1" EMT OR LARGER.
- ALL CONDUIT SHALL BE BONDED.
- ALL CONDUIT SHALL BE SUPPORTED BY RUBBER PYRAMID SUPPORTS AS REQUIRED.
- STEEL CITY 6"X6"X4" (OR LARGER) OUTDOOR JUNCTION BOXES SHALL BE USED AS NEEDED.
- ALL CONDUIT SHALL HAVE PLASTIC STRAIN RELIEF CONNECTORS TO PROTECT WIRES.

MARKING

- THE MATERIAL USED FOR MARKING SHALL BE REFLECTIVE, WEATHER RESISTANT AND SUITABLE FOR THE ENVIRONMENT. MARKING AS REQUIRED IN SECTIONS R 331.2.2 THROUGH R331.2.4 SHALL HAVE LETTERS CAPITALIZED WITH A MINIMUM HEIGHT OF 3/8" WHITE ON RED BACKGROUND. (CRC R331.2.1)
- THE MARKING CONTENT SHALL CONTAIN THE WORDS "WARNING; PHOTOVOLTAIC POWER SOURCE" (CRC R331.2.2)

NOTES

- WIRE AND SIZES ARE AS INDICATED OR LARGER. TEMPERATURE RATING WILL BE 90° C.
- ALL EQUIPMENT IS BONDED BY MECHANICAL MEANS OR BY A GROUNDING CONDUCTOR.
- THE GROUNDING ELECTRODE CONDUCTOR SHALL BE #8 AWG COPPER GROUND WIRE AND BONDED TO THE EXISTING GROUNDING ELECTRODE SYSTEM.
- CHANGES TO DESIGN NEED PRIOR ENGINEER APPROVAL.



renova.energy
Renova Energy Corp
75-181 Mediterranean Ave.
Palm Desert, CA 92211
(760)568-3413
Contractor's License #910836



PV Installation Company
Renova Energy Corp.
California - 2012



PANEL: LOAD CENTER
LOCATION: ROOFTOP
MOUNTING: SURFACE
CIRCUIT CODE: N: NON-CONTINUOUS
L: LONG-CONTINUOUS
R: DEMANDABLE RECEPTACLES
PANEL VOLTAGE: 480/277 V 3φ,4W
BUS: 250A

VA LOAD			LOAD DESCRIPTION	CODE	TYPE			C.B.			TYPE			CODE	LOAD DESCRIPTION	VA LOAD			
A	B	C			N	R	L	TR	P	A	B	C	P			TR	L	R	N
20833			INVERTER A	L			100	100	3	x					L				
	20833		INVERTER A	L			-	-	-		x				L				
		20833	INVERTER A	L			-	-	-			x			L				
20833			INVERTER B	L			100	100	3	x					L				
	20833		INVERTER B	L			-	-	-		x				L				
		20833	INVERTER B	L			-	-	-			x			L				
41666	41666	41666	PHASE TOTAL																
															PHASE TOTALS A+B+C		CONNECTED VA (CODE L):		124998
																	CONNECTED VA (CODE R):		
																	CONNECTED VA (CODE N):		
																	TOTAL CONNECTED KVA:		125.0
																	TOTAL PANEL AMPS:		159.0

PROJECT SIGNAGE

NOTES & PANEL SCHEDULE

Project Name:
NAUTILUS ABRASIVES BLASTING AND POWDER COATING LLC (COM19799)
14451 COMMERCE CENTER DRIVE
MORENO VALLEY, CA. 92553

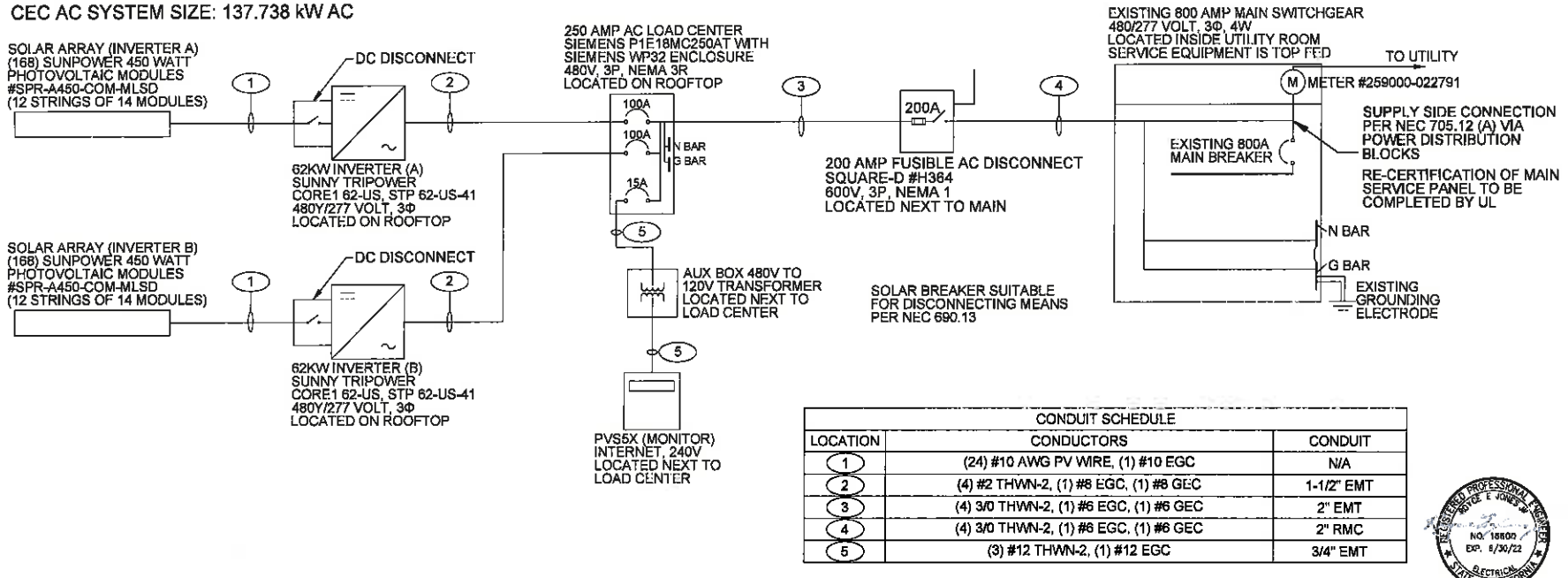
Drawn By: *DT*
DAVID THORNBURY

Date Drawn:
01/04/2021

Revision No.
0

Sheet No.
E1
PROJECT SIGNAGE

SOLAR SYSTEM SIZE: 151.200 kW DC
CEC AC SYSTEM SIZE: 137.738 kW AC



ALL WIRING BETWEEN MODULES SHALL BE TYPE PV-WIRE.
ALL CONDUIT SHALL BE BONDED.
STEEL CITY 12"x12"x4" (OR LARGER) OUTDOOR JUNCTION BOXES SHALL BE USED AS NEEDED.
ALL CONDUIT SHALL HAVE PLASTIC STRAIN RELIEF CONNECTORS TO PROTECT WIRES.

SINGLE LINE DIAGRAM

DC CIRCUIT CALCULATIONS

MAX VOLTAGE DC = (MODULE Voc + ((25°C - Temp Record Low°C [via www.solarabcs.org]) x T coefV)) X # modules in series
MAX VOLTAGE DC = (51.9 + ((25°C - 3°C) x .1360)) X 14 modules in series = 768.5 VOLTS
11.0 AMPS EACH CIRCUIT X 1.56 [PER NEC 690.8] = 17.2 AMPS
DC CONDUIT SHALL BE MOUNTED A MINIMUM OF 1" ABOVE ROOFTOP SURFACE.
ENGINEERED FOR 90°C TEMP. RATING OF CONDUCTOR & 44°C AMBIENT TEMP. PER SOLARABCS.ORG

MODULES TO INVERTER A

12 STRINGS X 14 MODULES = 168 MODULES
#10 WIRE = 40 AMPS X .82 [PER NEC TABLE 310.15(B)(16) & PER NEC TABLE 690.31(A)] = 32.8 AMPS ≥ 17.2 AMPS ✓
INVERTER A: 12 STRINGS = (24) #10 AWG PV WIRE, (1) #10 THWN-2 GROUND
PV WIRE RUNS UNDER SOLAR MODULES, SUPPORTED BY HELIX ROOF CABLE MANAGEMENT SYSTEM

MODULES TO INVERTER B

12 STRINGS X 14 MODULES = 168 MODULES
#10 WIRE = 40 AMPS X .82 [PER NEC TABLE 310.15(B)(16) & PER NEC TABLE 690.31(A)] = 32.8 AMPS
32.8 AMPS X .90 [CONDUIT FILL PER NEC TABLE 310.15(B)(3)(a)] = 26.24 AMPS > 17.2 AMPS ✓
INVERTER B: 12 STRINGS = (24) #10 AWG PV WIRE, (1) #10 THWN-2 GROUND
2 STRINGS PER CONDUIT (TRANSITION FROM ARRAY - SEE SHEET P1) =
(4) #10 AWG PV WIRE, (1) #10 THWN-2 BARE GROUND EGC
1" EMT @ 40% FILL PER NEC TABLE C.1 = (16) #10 THWN-2 ✓
PV WIRE RUNS UNDER SOLAR MODULES, SUPPORTED BY HELIX ROOF CABLE MANAGEMENT SYSTEM

INVERTER A TO 250A AC LOAD CENTER

INVERTER A MAX OUTPUT = 79.5A X 1.25 [PER NEC 215.2(A)1] = 99.4 AMPS (100 AMP SOLAR BREAKER)
#2 WIRE = 130 AMPS X .82 [PER NEC TABLE 310.15(B)(16)] = 106.5 AMP ≥ 100 AMP ✓
(4) #2 THWN-2 AWG WIRE, (1) #8 THWN-2 GROUND, (1) #8 THWN-2 GROUND
1-1/2" EMT @ 40% FILL PER NEC TABLE C.1 = (7) #2 THWN-2 WIRES ✓

INVERTER B TO 250A AC LOAD CENTER

INVERTER B MAX OUTPUT = 79.5A X 1.25 [PER NEC 215.2(A)1] = 99.4 AMPS (100 AMP SOLAR BREAKER)
#2 WIRE = 130 AMPS X .82 [PER NEC TABLE 310.15(B)(16)] = 106.5 AMP ≥ 100 AMP ✓
(4) #2 THWN-2 AWG WIRE, (1) #8 THWN-2 GROUND, (1) #8 THWN-2 GROUND
1-1/2" EMT @ 40% FILL PER NEC TABLE C.1 = (7) #2 THWN-2 WIRES ✓

250A AC LOAD CENTER TO MAIN PANEL

MAX OUTPUT = (79.5A + 79.5A) X 1.25 [PER NEC 215.2(A)1] = 198.8 AMPS (200 AMP FUSES)
AC CONDUIT SHALL BE MOUNTED ON EXTERIOR WALL.
3/0 WIRE = 225 AMPS ≥ 200 AMP ✓
(4) 3/0 THWN-2 AWG WIRE, (1) #6 THWN-2 GROUND, (1) #6 THWN-2 GROUND
2" EMT @ 40% FILL PER NEC TABLE C.1 = (5) 3/0 THWN-2 WIRES ✓

MAIN PANEL BACKFEED CALCULATION

EXISTING 800 AMP MAIN SWITCHGEAR
SUPPLY SIDE CONNECTION SHALL BE PERFORMED PER NEC 705.12(A)

renova.energy
Renova Energy Corp
75-181 Mediterranean Ave.
Palm Desert, CA 92211
(760) 568-3413
Contractor's License #910836

NABCEP ACCREDITED
PV Installation Company
Renova Energy Corp.
California - 2012

North American Board of
NABCEP

Project Name:
NAUTILUS ABRASIVES BLASTING AND POWDER COATING LLC (COM19799)
14451 COMMERCE CENTER DRIVE
MORENO VALLEY, CA. 92553

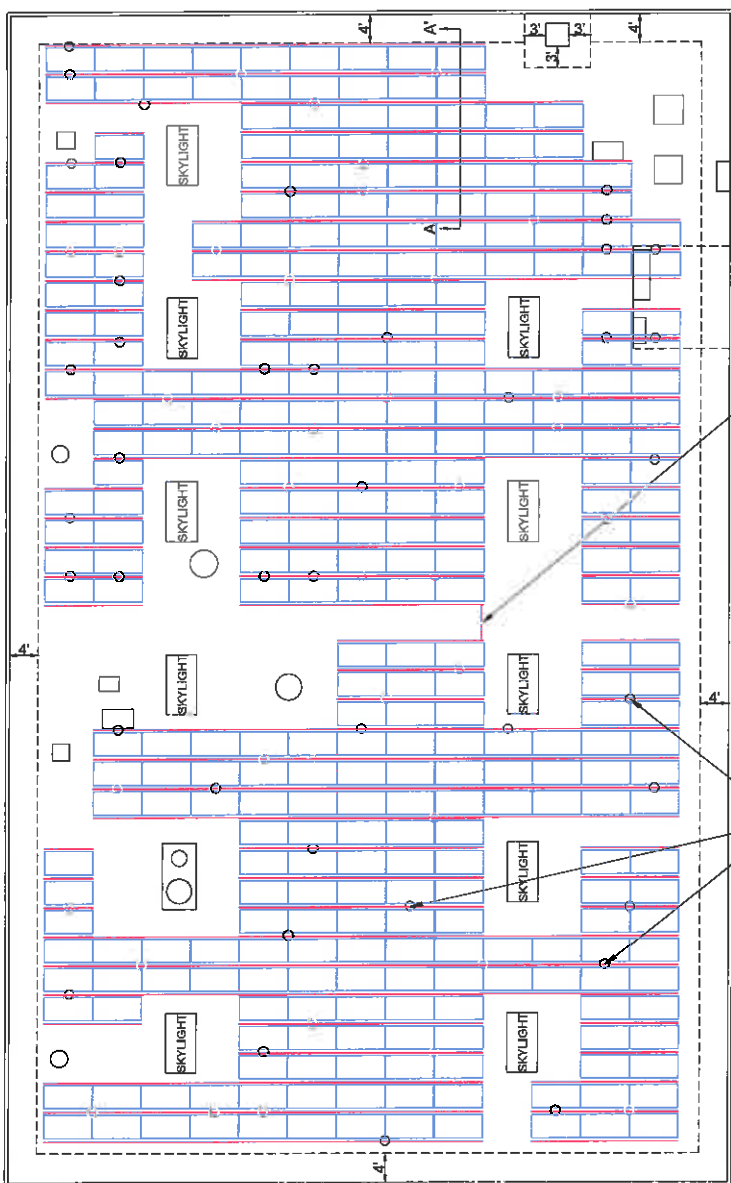
Drawn By: *DT*
DAVID THORNBURY

Date Drawn:
01/04/2021

Revision No.
0

Sheet No.
E2
SINGLE LINE

TOP VIEW



1" EMT CONDUIT FOR TRANSITION OF (2) CIRCUITS (CIRCUITS 13 & 14) FROM ARRAY TO ARRAY. SEE CALCULATIONS ("MODULES TO INVERTER B") ON SHEET E2

OMG POWERGRIP PLUS MECHANICAL ATTACHMENT SEE DETAIL ON SHEET P1 TYPICAL OF 83



*MODULES NOT TO EXCEED 18" ABOVE ROOF SURFACE

SCALE 1/4" = 1'

ELEVATION A-A'



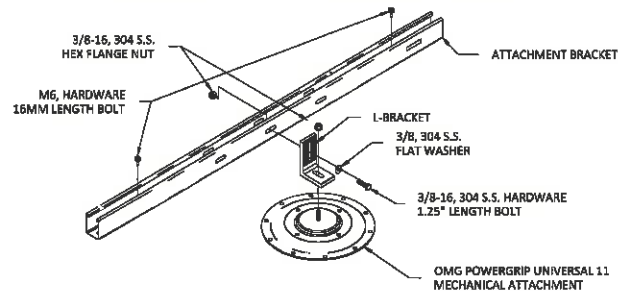
*MODULES NOT TO EXCEED 18" ABOVE ROOF SURFACE

SCALE 1/4" = 1'

*MODULES NOT TO EXCEED 18" ABOVE ROOF SURFACE

SCALE 1/4" = 1'

MECHANICAL ATTACHMENT DETAIL



*MODULES NOT TO EXCEED 18" ABOVE ROOF SURFACE

SCALE 1/4" = 1'

renova.energy
 Renova Energy Corp
 75-181 Mediterranean Ave.
 Palm Desert, CA 92211
 (760)568-3413
 Contractor's License #910935

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 PV Installation Company

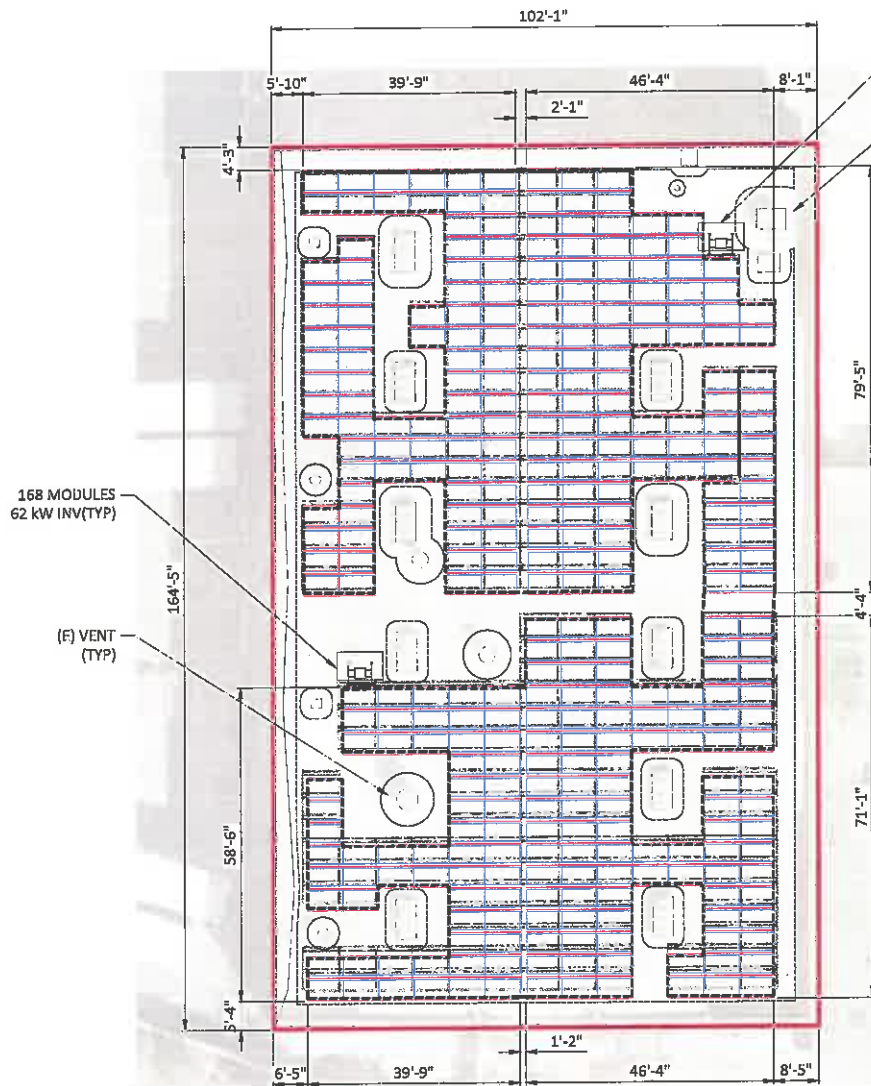
Project Name:
NAUTILUS ABRASIVES BLASTING AND POWDER COATING LLC (COM19799)
14451 COMMERCE CENTER DRIVE
MORENO VALLEY, CA. 92553

Drawn By: *DT*
DAVID THORNBURY

Date Drawn:
 01/04/2021

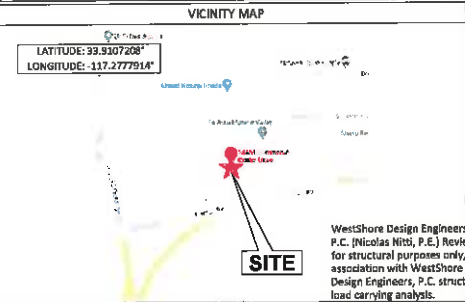
Revision No.
 0

Sheet No.
P1
PROFILE ELEVATIONS



- LAYOUT ASSUMES THAT ADDITION OF ARRAY WILL NOT INCREASE THE DEMAND/LOAD FACTOR RATIO OF ANY MEMBER IN THE BUILDING'S LATERAL FORCE RESISTING SYSTEM BY MORE THAN 10%, RELATIVE TO ORIGINAL CONSTRUCTION, PER IBC/CSC 3606.4.
- POINT OF INTERCONNECTION & METER @ TBD.
- THIS DESIGN COMPLIES WITH 2017 NEC RAPID SHUTDOWN REQUIREMENTS.
- STRING GROUPINGS WITHIN THE ARRAYS ARE REPRESENTATIVE AND MAY BE REDESIGNED BASED ON PARTNER'S STRING PLAN.

1 ARRAY LAYOUT
SCALE: 3/32" = 1'-0"



PROJECT SUMMARY	
DC SYSTEM SIZE (kW)	151.200
TOTAL # OF MODULE	336
MODULE TYPE	SPR-A450-COM-MLSD
AC SYSTEM SIZE (kW)	125.000
ESTIMATED YEAR 1 ENERGY OUTPUT	253,925.7 KWH
RACKING SYSTEM TYPE - ROOF COVER RATIO	HSTS-10-A-SERIES-77
AZIMUTH	0°
ROOF HEIGHT / PITCH	35' / ASSUMED FLAT

INVERTER SUMMARY				
ROOF	INVERTER TYPE	INVERTER QTY	QTY MODULES PER INVERTER	STRING CONFIGURATION
A	STP-CORE1 62-US-S (480V)	2	168	MPPT 1: 14 MODULES, 2 STRINGS
				MPPT 2: 14 MODULES, 2 STRINGS
				MPPT 3: 14 MODULES, 2 STRINGS
				MPPT 4: 14 MODULES, 2 STRINGS
				MPPT 5: 14 MODULES, 2 STRINGS
				MPPT 6: 14 MODULES, 2 STRINGS
TOTAL		2	336	

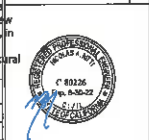
LEGEND	
	PROJECT SCOPE ROOFTOP BOUNDARY
	ROOF BOUNDARY AND ROOF OBSTRUCTION SETBACK
	SOLAR MODULES SINGLE TILT
	INVERTER POWERSTATION WITH WORKING CLEARANCE
	STRING GROUPING BOUNDARY



SUNPOWER® | HELIX L X²

THE PROPOSED ARRAY LAYOUT SHOWN IS DESIGNED TO FIT EXISTING CONDITIONS AS THEY ARE DESCRIBED ON THIS DRAWING. LAYOUT AND QUANTITIES ARE SUBJECT TO CHANGE BASED ON PARTNER/CONTRACTOR VERIFICATION OF ACTUAL SITE CONDITIONS.

SUNPOWER
1404 HARBOR WAY SOUTH
RICHMOND, CA 94804 USA
(510) 840-0950



RENOVA ENERGY CORP.
NATURAL GAS ENGINEERING & POWER RATING LLC
14451 COMMERCE CENTER DR
MORENO VALLEY, CA 92553, USA

REV	DATE	DESCRIPTION
A	11-18-20	PROVISIONAL
B	11-18-20	CHANGE RACKING
C	11-18-20	CHANGE RACKING
D	11-18-20	CHANGE RACKING

DATE DRAWN: 11-18-20
OPPORTUNITY #: CPE20-20247634

SHEET
SP 1.0



Corporate Headquarters
75-181 Mediterranean Ave. | Palm Desert, CA 92211

Engineering Report for Solar Glare Analysis

Nautilus Abrasives Blasting and Powder Coating, LLC., 14451
Commerce Center Drive, Moreno Valley, CA 92553

March 11, 2021

Renova Energy Corp. has a contract with Nautilus Abrasives Blasting and Powder Coating, LLC. located at 14451 Commerce Center Drive in Moreno Valley, CA 92553. The contract is for a new 151.2 kW ballasted solar photovoltaic system that shall be installed on the rooftop of the existing structure. The solar array is comprised of (336) SunPower SPR-A450-COM-MLSD 450 watt modules and a single-tilt ballasted PanelClaw mounting system. The specifications for the proposed solar photovoltaic system, photovoltaic modules and racking system are included in this package.

The project was submitted to the building department of the City of Moreno Valley and it was determined it was within range of the March Air Reserve Base (March AFB) and needed further approval from the Airport Land Use Commission. The Authority Having Jurisdiction requested a Solar Glare Hazard Analysis be completed in order to prove no excessive glint or glare will be created by the project to interfere with pilots or the control tower operating this facility.

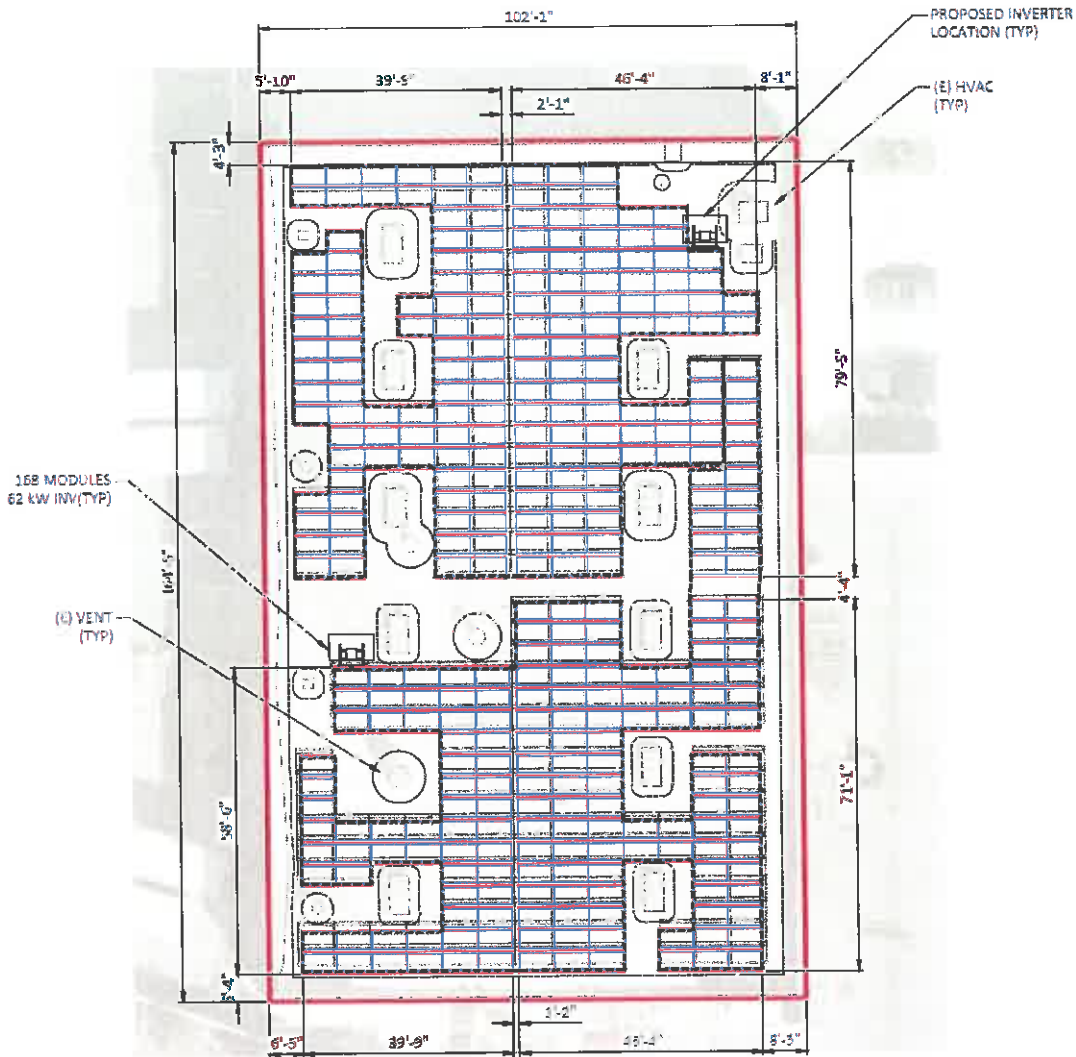
Utilizing ForgeSolar Glare Study tool, a final glare model and analysis was completed for the site and solar photovoltaic system. The results of the glare study found to PASS analyses compliant with the Airport Land Use Commission, FAA and USAF regulations in all three components provided including: Analysis Parameters, Flight Paths and ATCTs. The inputs, model parameters and results from the analysis are document and attached.

Summarized inputs and findings can be found below. Complete documents are included in the packet.

Vicinity Map



Solar PV System



ADDITION OF ARRAY WILL NOT INCREASE THE
LOAD ON THE BUILDING'S ELECTRICAL SYSTEM
OR THE BUILDING'S MECHANICAL SYSTEMS.
THESE FIGURES ARE APPROXIMATE AND SHOULD BE
VERIFIED BY THE CLIENT'S ELECTRICAL ENGINEER.

1 ARRAY LAYOUT
SCALE: 3/32" = 1'-0"

Summarized Solar Glare Inputs

SITE CONFIGURATION

Analysis Parameters

DNI: peaks at 1,000.0 W/m²
 Time interval: 1 min
 Ocular transmission coefficient: 0.5
 Pupil diameter: 0.002 m
 Eye focal length: 0.017 m
 Sun subtended angle: 9.3 mrad
 Site Config ID: 90639/9062



PV Array(s)

Name: PV array 1
 Axis tracking: Fixed (no rotation)
 Tilt: 5.0°
 Orientation: 180.0°
 Rated power: 151.2 kW
 Panel material: Smooth glass without AR coating
 Reflectivity: Vary with sun
 Slope error: correlate with material



Vertex	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
1	33.910685	-117.275883	1554.98	20.00	1574.98
2	33.910688	-117.275884	1555.98	20.00	1575.98
3	33.910490	-117.275880	1557.88	20.00	1577.88
4	33.910451	-117.275973	1559.08	20.00	1579.08



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Summarized Solar Glare Findings

FORGESOLAR GLARE ANALYSIS

Project: **Nautilus**

Proposed PV Rooftop (396) SunPower SPR-A450-COM-ML&D Hallix.

Site configuration: **South Single Tilt at 5 Degrees - 3-5-21**

Analysis conducted by Matthew De La Torre (mdelatorre@renovaenergy.com) at 22:09 on 05 Mar, 2021.

U.S. FAA 2013 Policy Adherence

The following table summarizes the policy adherence of the glare analysis based on the 2013 U.S. Federal Aviation Administration Interim Policy 78 FR 63276. This policy requires the following criteria be met for solar energy systems on airport property:

- No "yellow" glare (potential for after-image) for any flight path from threshold to 2 miles
- No glare of any kind for Air Traffic Control Tower(s) ("ATCT") at cab height.
- Default analysis and observer characteristics (see list below)

ForgeSolar does not represent or speak officially for the FAA and cannot approve or deny projects. Results are informational only.

COMPONENT	STATUS	DESCRIPTION
Analysis parameters	PASS	Analysis time interval and eye characteristics used are acceptable
2-mile flight path(s)	PASS	Flight path receptor(s) do not receive yellow glare
ATCT(s)	PASS	Receptor(s) marked as ATCT do not receive glare



FORGESOLAR GLARE ANALYSIS

Project: **Nautilus**

Proposed PV Rooftop (336) SunPower SPR-A450-COM-MLSD Helix.

Site configuration: **South Single Tilt at 5 Degrees - 3-5-21**

Analysis conducted by Matthew De La Torre (mdelatorre@renovaenergy.com) at 22:09 on 05 Mar, 2021.

U.S. FAA 2013 Policy Adherence

The following table summarizes the policy adherence of the glare analysis based on the 2013 U.S. Federal Aviation Administration Interim Policy 78 FR 63276. This policy requires the following criteria be met for solar energy systems on airport property:

- No "yellow" glare (potential for after-image) for any flight path from threshold to 2 miles
- No glare of any kind for Air Traffic Control Tower(s) ("ATCT") at cab height.
- Default analysis and observer characteristics (see list below)

ForgeSolar does not represent or speak officially for the FAA and cannot approve or deny projects. Results are Informational only.

COMPONENT	STATUS	DESCRIPTION
Analysis parameters	PASS	Analysis time interval and eye characteristics used are acceptable
2-mile flight path(s)	PASS	Flight path receptor(s) do not receive yellow glare
ATCT(s)	PASS	Receptor(s) marked as ATCT do not receive glare

Default glare analysis parameters and observer eye characteristics (for reference only):

- Analysis time interval: 1 minute
- Ocular transmission coefficient: 0.5
- Pupil diameter: 0.002 meters
- Eye focal length: 0.017 meters
- Sun subtended angle: 9.3 milliradians

FAA Policy 78 FR 63276 can be read at <https://www.federalregister.gov/d/2013-24729>

SITE CONFIGURATION

Analysis Parameters

DNI: peaks at 1,000.0 W/m²
Time interval: 1 min
Ocular transmission coefficient: 0.5
Pupil diameter: 0.002 m
Eye focal length: 0.017 m
Sun subtended angle: 9.3 mrad
Site Config ID: 50639.9052



PV Array(s)

Name: PV array 1
Axis tracking: Fixed (no rotation)
Tilt: 5.0°
Orientation: 180.0°
Rated power: 151.2 kW
Panel material: Smooth glass without AR coating
Reflectivity: Vary with sun
Slope error: correlate with material



Vertex	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
1	33.910885	-117.275868	1554.38	20.00	1574.38
2	33.910888	-117.275551	1555.98	20.00	1575.98
3	33.910450	-117.275550	1557.88	20.00	1577.88
4	33.910451	-117.275873	1556.08	20.00	1576.08

Flight Path Receptor(s)

Name: C/KC, Rwy 14 Base

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.922394	-117.325047	1500.07	1500.07	3000.15
Two-mile	33.931244	-117.309014	1500.07	1500.07	3000.15

Name: C/KC Rwy 14 Crosswind

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.821961	-117.228367	1500.07	1500.07	3000.15
Two-mile	33.813147	-117.244350	1500.07	1500.07	3000.15

Name: C/KC, Rwy 14 Downwind

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



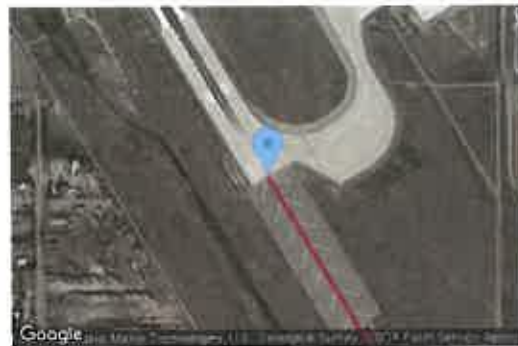
Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.819225	-117.262269	1500.07	1500.07	3000.15
Two-mile	33.908131	-117.325528	1500.07	1500.07	3000.15

Name: C/KC, Rwy 14 Final
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.925156	-117.291061	1500.07	1500.07	3000.15
Two-mile	33.896431	-117.270636	1500.07	0.00	1500.07

Name: C/KC, Rwy 14 Upwind
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.864994	-117.248281	1500.07	0.00	1500.07
Two-mile	33.836269	-117.227869	1500.07	1500.07	3000.15

Name: C/KC, Rwy 32 Base
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.813147	-117.244350	1500.07	1500.07	3000.15
Two-mile	33.821961	-117.228367	1500.07	1500.07	3000.15

Name: C/KC, Rwy 32 Crosswind

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.931244	-117.309014	1500.07	1500.07	3000.15
Two-mile	33.922394	-117.325047	1500.07	1500.07	3000.15

Name: C/KC, Rwy 32 Downwind

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.908131	-117.325528	1500.07	1500.07	3000.15
Two-mile	33.819225	-117.262269	1500.07	1500.07	3000.15

Name: C/KC, Rwy 32 Final

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.836269	-117.227869	1500.07	1500.07	3000.15
Two-mile	33.864994	-117.248281	1500.07	0.00	1500.07

Name: C/KC, Rwy 32 Upwind

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.896431	-117.270636	1500.07	0.00	1500.07
Two-mile	33.925156	-117.291061	1500.07	1500.07	3000.15

Name: Ga, Rwy 12 Base

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.910322	-117.264967	1500.07	1300.06	2800.14
Two-mile	33.905592	-117.270622	1500.07	1300.06	2800.14

Name: GA, Rwy 12 Crosswind

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.876081	-117.235119	1500.07	1300.06	2800.14
Two-mile	33.880814	-117.229467	1500.07	1300.06	2800.14

Name: GA, Rwy 12 Downwind
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.887897	-117.229483	1500.07	1300.06	2800.14
Two-mile	33.910333	-117.256469	1500.07	1300.06	2800.14

Name: GA, Rwy 12 Final
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.898508	-117.270608	1500.07	1300.06	2800.14
Two-mile	33.890258	-117.260681	1500.07	0.00	1500.07

Name: GA, Rwy 14 Base
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.904833	-117.292903	1500.07	1500.07	3000.15
Two-mile	33.908242	-117.286017	1500.07	1500.07	3000.15

Name: GA, Rwy 14 Crosswind
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.848078	-117.243236	1500.07	1500.07	3000.15
Two-mile	33.844669	-117.250119	1500.07	1500.07	3000.15

Name: GA, Rwy 14 Downwind
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.846422	-117.258344	1500.07	1500.07	3000.15
Two-mile	33.897972	-117.295011	1500.07	1500.07	3000.15

Name: GA, Rwy 14 Final
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.906486	-117.277783	1500.07	1500.07	3000.15
Two-mile	33.896431	-117.270636	1500.07	0.00	1500.07

Name: GA, Rwy 14 Upwind
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.864994	-117.248281	1500.07	0.00	1500.07
Two-mile	33.854942	-117.241136	1500.07	1500.07	3000.15

Name: GA, Rwy 30 Base
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.880814	-117.229467	1500.07	1300.06	2800.14
Two-mile	33.876081	-117.235119	1500.07	1300.06	2800.14

Name: Ga, Rwy 30 Crosswind
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.905592	-117.270622	1500.07	1300.06	2800.14
Two-mile	33.910322	-117.264967	1500.07	1300.06	2800.14

Name: Ga, Rwy 30 Downwind

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.910333	-117.256469	1500.07	1300.06	2800.14
Two-mile	33.887897	-117.229483	1500.07	1300.06	2800.14

Name: GA, Rwy 30 Final

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.876069	-117.243611	1500.07	1300.06	2800.14
Two-mile	33.884319	-117.253536	1500.07	0.00	1500.07

Name: Ga, Rwy 30 Upwind

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.890258	-117.260681	1500.07	0.00	1500.07
Two-mile	33.898508	-117.270608	1500.07	1300.06	2800.14

Name: GA, Rwy 32 Base

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.844669	-117.250119	1500.07	1500.07	3000.15
Two-mile	33.848078	-117.243236	1500.07	1500.07	3000.15

Name: Ga, Rwy 32 Crosswind

Description: None

Threshold height: 0 ft

Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.908242	-117.286017	1500.07	1500.07	3000.15
Two-mile	33.904833	-117.292903	1500.07	1500.07	3000.15

Name: Ga, Rwy 32 Downwind

Description: None

Threshold height: 0 ft

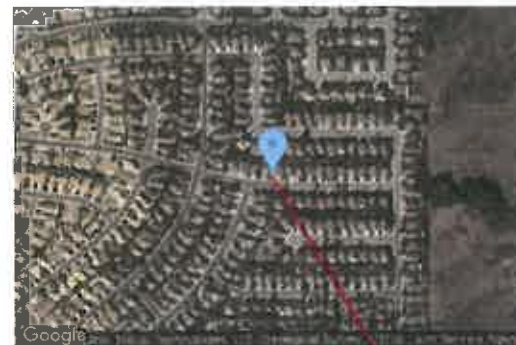
Direction: 314.8°

Glide slope: 5.0°

Pilot view restricted? Yes

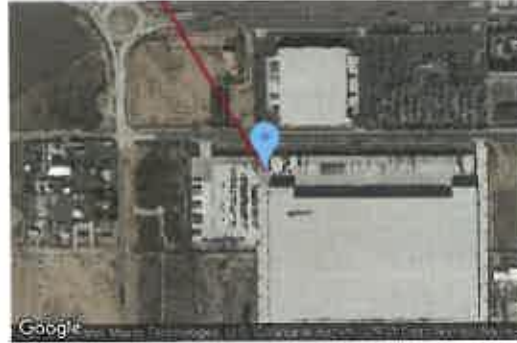
Vertical view: 30.0°

Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.897972	-117.295011	1500.07	1500.07	3000.15
Two-mile	33.846422	-117.258344	1500.07	1500.07	3000.15

Name: GA, Rwy 32 Final
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.854942	-117.241136	1500.07	1500.07	3000.15
Two-mile	33.864994	-117.248281	1500.07	0.00	1500.07

Name: GA, Rwy 32 Upwind
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.896431	-117.270636	1500.07	0.00	1500.07
Two-mile	33.906486	-117.277783	1500.07	1500.07	3000.15

Name: O'Head, Rwy 14 Downwind
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.863564	-117.293808	1500.07	2000.10	3500.17
Two-mile	33.908131	-117.325528	1500.07	2000.10	3500.17

Name: OHead, Rwy 14 Final
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.925156	-117.291061	1500.07	2000.10	3500.17
Two-mile	33.896431	-117.270636	1500.07	0.00	1500.07

Name: OHead, Rwy 14 Initial
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.968036	-117.322128	1500.07	2000.10	3500.17
Two-mile	33.880706	-117.259453	1500.07	2000.10	3500.17

Name: OHead, Rwy 32 Downwind
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.863564	-117.293808	1500.07	2000.10	3500.17
Two-mile	33.819225	-117.262269	1500.07	2000.10	3500.17

Name: OHead, Rwy 32 Final
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.836269	-117.227869	1500.07	2000.10	3500.17
Two-mile	33.864994	-117.248281	1500.07	0.00	1500.07

Name: OHead, Rwy 32 Initial
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.793375	-117.196878	1500.07	2000.10	3500.17
Two-mile	33.880706	-117.259453	1500.07	2000.10	3500.17

Name: Rwy 12-Upwind
Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°
Pilot view restricted? Yes
Vertical view: 30.0°
Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.884319	-117.253536	1500.07	0.00	1500.07
Two-mile	33.876069	-117.243611	1500.07	1300.06	2800.14

Discrete Observation Receptors

Name	ID	Latitude (°)	Longitude (°)	Elevation (ft)	Height (ft)
1-ATCT	1	33.891564	-117.251171	1508.77	18.00

Map image of 1-ATCT



GLARE ANALYSIS RESULTS

Summary of Glare

PV Array Name	Tilt (°)	Orient (°)	"Green" Glare min	"Yellow" Glare min	Energy kWh
PV array 1	5.0	180.0	7,632	0	325,700.0

Total annual glare received by each receptor

Receptor	Annual Green Glare (min)	Annual Yellow Glare (min)
C/KC, Rwy 14 Base	0	0
C/KC Rwy 14 Crosswind	0	0
C/KC, Rwy 14 Downwind	0	0
C/KC, Rwy 14 Final	0	0
C/KC, Rwy 14 Upwind	0	0
C/KC, Rwy 32 Base	0	0
C/KC, Rwy 32 Crosswind	36	0
C/KC, Rwy 32 Downwind	0	0
C/KC, Rwy 32 Final	0	0
C/KC, Rwy 32 Upwind	2456	0
Ga, Rwy 12 Base	0	0

Receptor	Annual Green Glare (min)	Annual Yellow Glare (min)
GA, Rwy 12 Crosswind	0	0
GA, Rwy 12 Downwind	0	0
GA, Rwy 12 Final	0	0
GA, Rwy 14 Base	0	0
GA, Rwy 14 Crosswind	0	0
GA, Rwy 14 Downwind	0	0
GA, Rwy 14 Final	0	0
GA, Rwy 14 Upwind	0	0
GA, Rwy 30 Base	0	0
Ga, Rwy 30 Crosswind	310	0
Ga, Rwy 30 Downwind	1888	0
GA, Rwy 30 Final	0	0
Ga, Rwy 30 Upwind	0	0
GA, Rwy 32 Base	0	0
Ga, Rwy 32 Crosswind	2942	0
Ga, Rwy 32 Downwind	0	0
GA, Rwy 32 Final	0	0
GA, Rwy 32 Upwind	0	0
OHead, Rwy 14 Downwind	0	0
OHead, Rwy 14 Final	0	0
OHead, Rwy 14 Initial	0	0
OHead, Rwy 32 Downwind	0	0
OHead, Rwy 32 Final	0	0
OHead, Rwy 32 Initial	0	0
Rwy 12-Upwind	0	0
1-ATCT	0	0

Results for: PV array 1

Receptor	Green Glare (min)	Yellow Glare (min)
C/KC, Rwy 14 Base	0	0
C/KC Rwy 14 Crosswind	0	0
C/KC, Rwy 14 Downwind	0	0
C/KC, Rwy 14 Final	0	0
C/KC, Rwy 14 Upwind	0	0
C/KC, Rwy 32 Base	0	0
C/KC, Rwy 32 Crosswind	36	0
C/KC, Rwy 32 Downwind	0	0
C/KC, Rwy 32 Final	0	0
C/KC, Rwy 32 Upwind	2456	0

Receptor	Green Glare (min)	Yellow Glare (min)
Ga, Rwy 12 Base	0	0
GA, Rwy 12 Crosswind	0	0
GA, Rwy 12 Downwind	0	0
GA, Rwy 12 Final	0	0
GA, Rwy 14 Base	0	0
GA, Rwy 14 Crosswind	0	0
GA, Rwy 14 Downwind	0	0
GA, Rwy 14 Final	0	0
GA, Rwy 14 Upwind	0	0
GA, Rwy 30 Base	0	0
Ga, Rwy 30 Crosswind	310	0
Ga, Rwy 30 Downwind	1888	0
GA, Rwy 30 Final	0	0
Ga, Rwy 30 Upwind	0	0
GA, Rwy 32 Base	0	0
Ga, Rwy 32 Crosswind	2942	0
Ga, Rwy 32 Downwind	0	0
GA, Rwy 32 Final	0	0
GA, Rwy 32 Upwind	0	0
OHead, Rwy 14 Downwind	0	0
OHead, Rwy 14 Final	0	0
OHead, Rwy 14 Initial	0	0
OHead, Rwy 32 Downwind	0	0
OHead, Rwy 32 Final	0	0
OHead, Rwy 32 Initial	0	0
Rwy 12-Upwind	0	0
1-ATCT	0	0

Flight Path: C/KC, Rwy 14 Base

0 minutes of yellow glare

0 minutes of green glare

Flight Path: C/KC Rwy 14 Crosswind

0 minutes of yellow glare

0 minutes of green glare

Flight Path: C/KC, Rwy 14 Downwind

0 minutes of yellow glare

0 minutes of green glare

Flight Path: C/KC, Rwy 14 Final

0 minutes of yellow glare
 0 minutes of green glare

Flight Path: C/KC, Rwy 14 Upwind

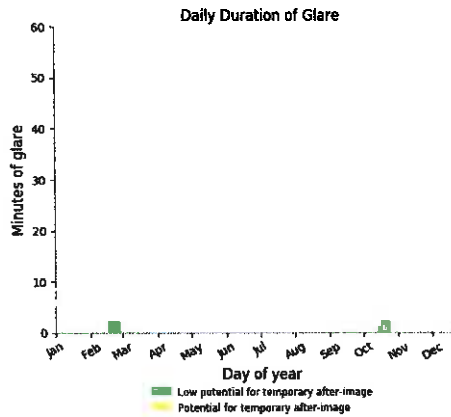
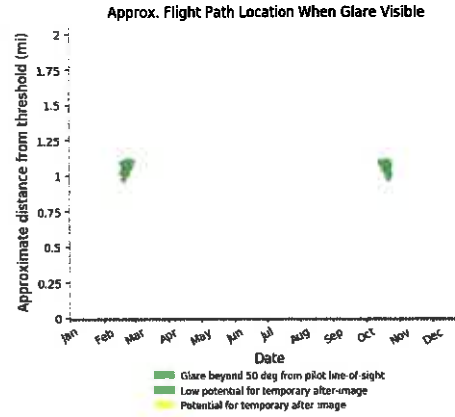
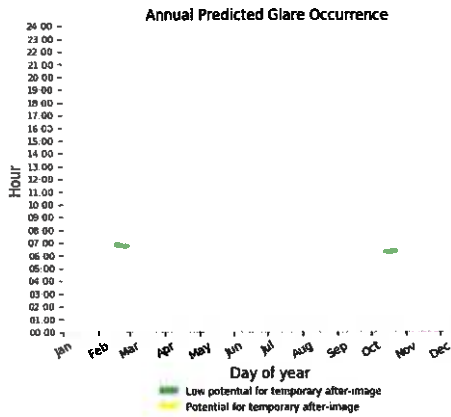
0 minutes of yellow glare
 0 minutes of green glare

Flight Path: C/KC, Rwy 32 Base

0 minutes of yellow glare
 0 minutes of green glare

Flight Path: C/KC, Rwy 32 Crosswind

0 minutes of yellow glare
 36 minutes of green glare



Flight Path: C/KC, Rwy 32 Downwind

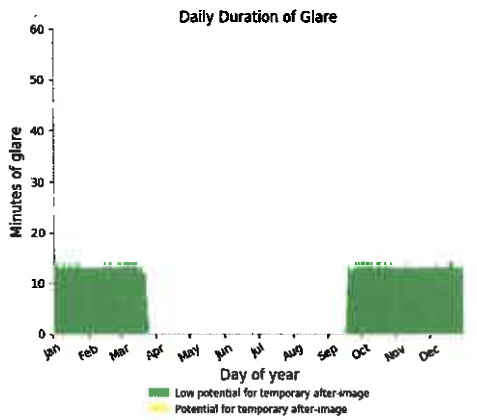
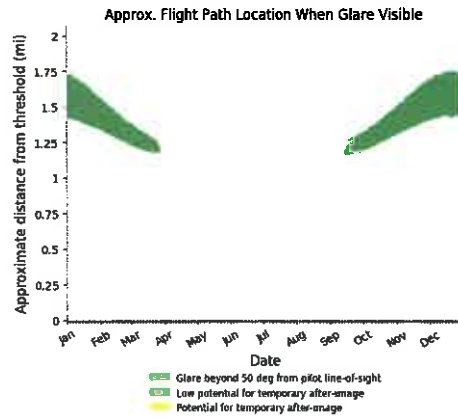
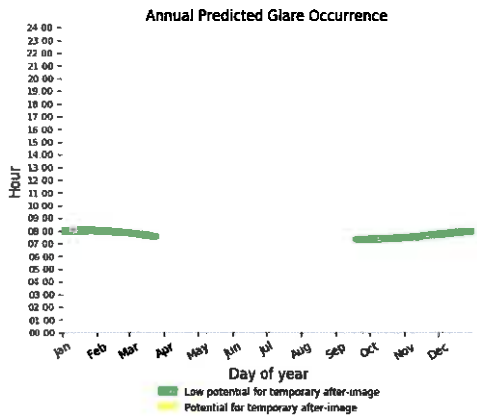
0 minutes of yellow glare
 0 minutes of green glare

Flight Path: C/KC, Rwy 32 Final

0 minutes of yellow glare
 0 minutes of green glare

Flight Path: C/KC, Rwy 32 Upwind

0 minutes of yellow glare
 2456 minutes of green glare



Flight Path: Ga, Rwy 12 Base

0 minutes of yellow glare
 0 minutes of green glare

Flight Path: GA, Rwy 12 Crosswind

0 minutes of yellow glare

0 minutes of green glare

Flight Path: GA, Rwy 12 Downwind

0 minutes of yellow glare

0 minutes of green glare

Flight Path: GA, Rwy 12 Final

0 minutes of yellow glare

0 minutes of green glare

Flight Path: GA, Rwy 14 Base

0 minutes of yellow glare

0 minutes of green glare

Flight Path: GA, Rwy 14 Crosswind

0 minutes of yellow glare

0 minutes of green glare

Flight Path: GA, Rwy 14 Downwind

0 minutes of yellow glare

0 minutes of green glare

Flight Path: GA, Rwy 14 Final

0 minutes of yellow glare

0 minutes of green glare

Flight Path: GA, Rwy 14 Upwind

0 minutes of yellow glare

0 minutes of green glare

Flight Path: GA, Rwy 30 Base

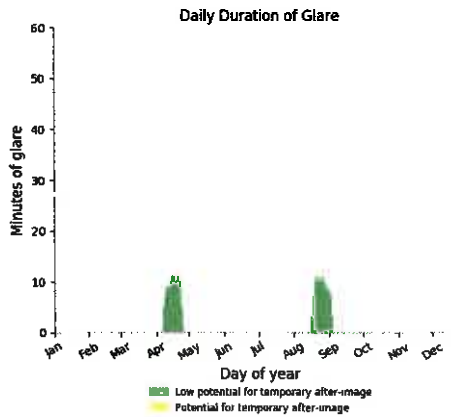
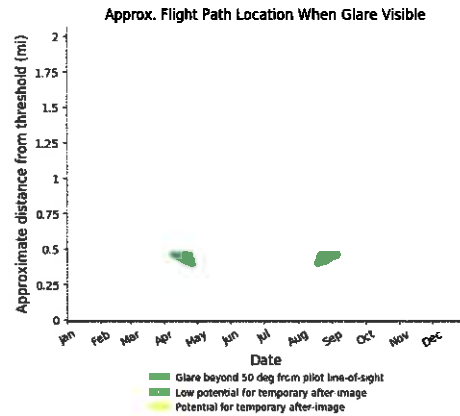
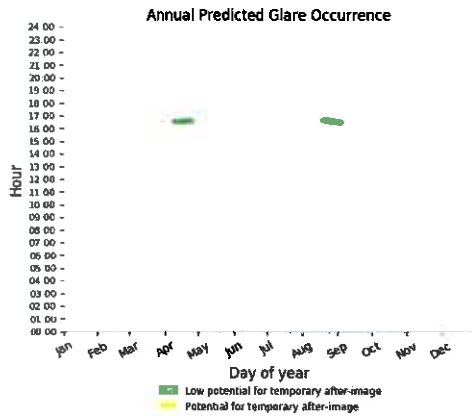
0 minutes of yellow glare

0 minutes of green glare

Flight Path: Ga, Rwy 30 Crosswind

0 minutes of yellow glare

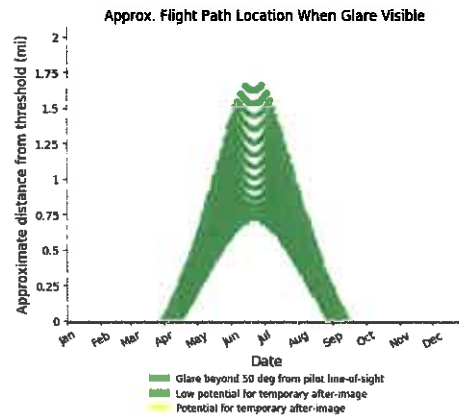
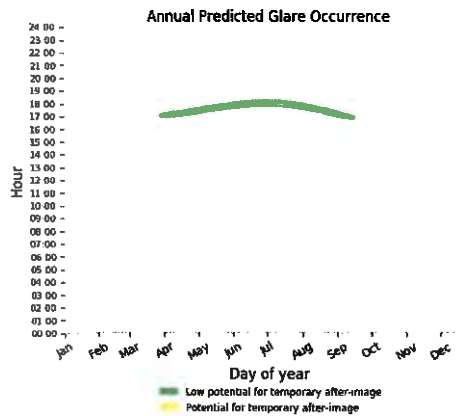
310 minutes of green glare

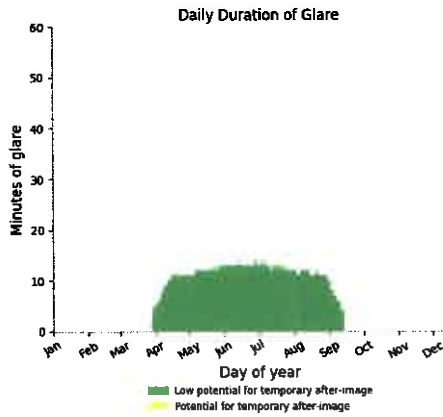


Flight Path: Ga, Rwy 30 Downwind

0 minutes of yellow glare

1888 minutes of green glare





Flight Path: GA, Rwy 30 Final

0 minutes of yellow glare
 0 minutes of green glare

Flight Path: Ga, Rwy 30 Upwind

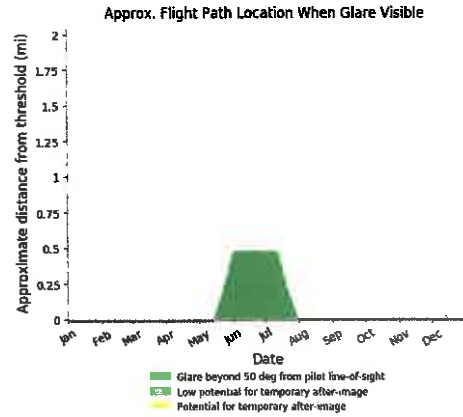
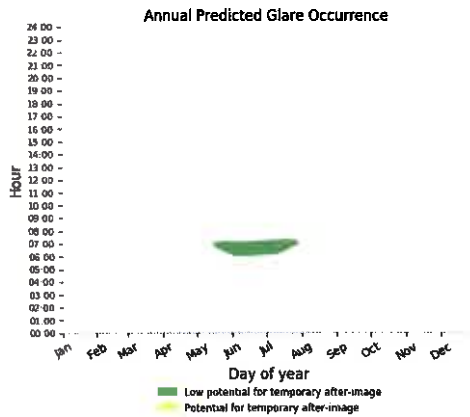
0 minutes of yellow glare
 0 minutes of green glare

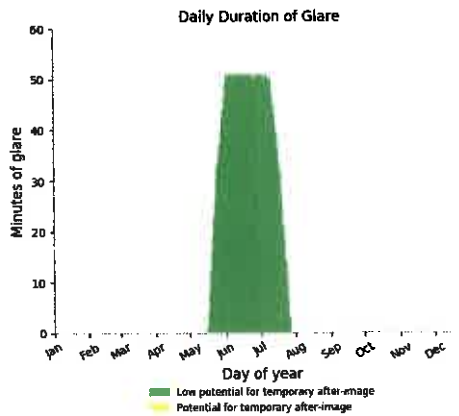
Flight Path: GA, Rwy 32 Base

0 minutes of yellow glare
 0 minutes of green glare

Flight Path: Ga, Rwy 32 Crosswind

0 minutes of yellow glare
 2942 minutes of green glare





Flight Path: Ga, Rwy 32 Downwind

0 minutes of yellow glare
 0 minutes of green glare

Flight Path: GA, Rwy 32 Final

0 minutes of yellow glare
 0 minutes of green glare

Flight Path: GA, Rwy 32 Upwind

0 minutes of yellow glare
 0 minutes of green glare

Flight Path: OHead, Rwy 14 Downwind

0 minutes of yellow glare
 0 minutes of green glare

Flight Path: OHead, Rwy 14 Final

0 minutes of yellow glare
 0 minutes of green glare

Flight Path: OHead, Rwy 14 Initial

0 minutes of yellow glare
 0 minutes of green glare

Flight Path: OHead, Rwy 32 Downwind

0 minutes of yellow glare
 0 minutes of green glare

Flight Path: OHead, Rwy 32 Final

0 minutes of yellow glare

0 minutes of green glare

Flight Path: OHead, Rwy 32 Initial

0 minutes of yellow glare

0 minutes of green glare

Flight Path: Rwy 12-Upwind

0 minutes of yellow glare

0 minutes of green glare

Point Receptor: 1-ATCT

0 minutes of yellow glare

0 minutes of green glare

Assumptions

"Green" glare is glare with low potential to cause an after-image (flash blindness) when observed prior to a typical blink response time.

"Yellow" glare is glare with potential to cause an after-image (flash blindness) when observed prior to a typical blink response time.

Times associated with glare are denoted in Standard time. For Daylight Savings, add one hour.

Glare analyses do not account for physical obstructions between reflectors and receptors. This includes buildings, tree cover and geographic obstructions.

Several calculations utilize the PV array centroid, rather than the actual glare spot location, due to algorithm limitations. This may affect results for large PV footprints. Additional analyses of array sub-sections can provide additional information on expected glare.

The subtended source angle (glare spot size) is constrained by the PV array footprint size. Partitioning large arrays into smaller sections will reduce the maximum potential subtended angle, potentially impacting results if actual glare spots are larger than the sub-array size.

Additional analyses of the combined area of adjacent sub-arrays can provide more information on potential glare hazards. (See previous point on related limitations.)

Glare locations displayed on receptor plots are approximate. Actual glare-spot locations may differ.

Glare vector plots are simplified representations of analysis data. Actual glare emanations and results may differ.

The glare hazard determination relies on several approximations including observer eye characteristics, angle of view, and typical blink response time. Actual results and glare occurrence may differ.

Hazard zone boundaries shown in the Glare Hazard plot are an approximation and visual aid based on aggregated research data. Actual ocular impact outcomes encompass a continuous, not discrete, spectrum.

Refer to the Help page at www.forgesolar.com/help/ for assumptions and limitations not listed here.

NOTICE OF PUBLIC HEARING
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION
www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact **ALUC Planner Paul Rull at (951) 955-6893**. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The City of Moreno Valley Building and Safety Division should be contacted on non-ALUC issues. For more information please contact City of Moreno Valley Building and Safety contact Ms. Grace Espino-Salcedo at (951) 413-3451.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to prull@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please telephone Barbara Santos at (951) 955-5132.

PLACE OF HEARING: **Riverside County Administration Center**
 4080 Lemon Street, 1st Floor Board Chambers
 Riverside California

DATE OF HEARING: **May 13, 2021**

TIME OF HEARING: **9:30 A.M.**

Pursuant to Executive Order N-25-20, this meeting will be conducted by teleconference and at the Place of Hearing, as listed above. Public access to the meeting location will be allowed, but limited to comply with the Executive Order. Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org

CASE DESCRIPTION:

ZAP1462MA21 – Nautilus Abrasives Blasting and Powder Coating, LLC (Representative: Renova Energy) – City of Moreno Valley Case No. BAP21-0330 (Building Permit), a proposal to construct a 7,345 square foot rooftop solar panel system on an existing industrial building on 1.0 acre located at 14451 Commerce Center Drive, northerly of Cactus Avenue, easterly of Old 215 Frontage Road, and southerly of Alessandro Boulevard (Airport Compatibility Zones B1-APZ-I and B2 of the March Air Reserve Base/Inland Port Airport Influence Area).



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER: ZAP1462MAZ1 DATE SUBMITTED: 03/16/21

APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION

Applicant	Renova Energy	Phone Number	760-568-3413
Mailing Address	75181 Mediterranean Ave Palm Desert, CA 92211	Email	permits@renovaenergy.com
Representative	Renova Energy - Jamie Silva	Phone Number	760-368-8033
Mailing Address	75181 Mediterranean Ave Palm Desert, CA 92211	Email	permits@renovaenergy.com
Property Owner	Nautilus Abrasives Blasting and Powder Coating LLC	Phone Number	951-842-6260
Mailing Address	14451 Commerce Center Drive Moreno Valley, CA 92553	Email	cphilips@nautiluspowdercoating.com

LOCAL JURISDICTION AGENCY

Local Agency Name	City of Moreno Valley	Phone Number	951-413-3451
Staff Contact	gracee@moval.org <u>GRACE BRINO-SALCEDO</u>	Email	gracee@moval.org
Mailing Address	14177 Frederick St Moreno Valley, CA 92553	Case Type	<input checked="" type="checkbox"/> General Plan / Specific Plan Amendment <input type="checkbox"/> Zoning Ordinance Amendment <input type="checkbox"/> Subdivision Parcel Map / Tentative Tract <input type="checkbox"/> Use Permit <input type="checkbox"/> Site Plan Review/Plot Plan <input type="checkbox"/> Other
Local Agency Project No	BAP21-0330		

PROJECT LOCATION

Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways

Street Address	14451 Commerce Center Drive Moreno Valley, CA 92553	Gross Parcel Size	1 Acre
Assessor's Parcel No.	297-130-062	Nearest Airport and distance from Airport	March Air Reserve Base - 275 ft.
Subdivision Name	PM 32392		
Lot Number	3		

PROJECT DESCRIPTION

If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed

Existing Land Use (describe) 17,000 Sq. Ft. Commercial/Industrial Building used for powder and liquid industrial painting and coatings.

B1-ADZ-1
B2
March

Proposed Land Use (describe)	Renova Energy Corp. is proposing to install a 7,345 Sq. Ft. Solar Photovoltaic System on the existing roof of the commercial building. This will be a ballasted system, facing south at a 5 degree tilt angle.		
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units) _____		
For Other Land Uses (See Appendix C)	Hours of Operation	6am -2:30 pm, Monday -Friday	
	Number of People on Site	10	Maximum Number 85
	Method of Calculation	17,000 SF Building / 100 sq. ft./occupant = 170 people max (under CBC). 170 people max x 50% adjustment = 85 people maximum per Appendix C.	
Height Data	Site Elevation (above mean sea level)	1555	ft.
	Height of buildings or structures (from the ground)	20' - Solar panels to add 12"	ft.
Flight Hazards	Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight?	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	
	If yes, describe	Solar Panel Glare	

- A. **NOTICE:** Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. **REVIEW TIME:** Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. **SUBMISSION PACKAGE:**
1. Completed ALUC Application Form
 1. ALUC fee payment
 1. Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
 1. Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
 1. CD with digital files of the plans (pdf)
 1. Vicinity Map (8.5x11)
 1. Detailed project description
 1. Local jurisdiction project transmittal
 3. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
 3. Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. **(Only required if the project is scheduled for a public hearing Commission meeting)**

HYBRID INTENSITY POLICY FOR PARCEL DELIVERY FACILITIES

BACKGROUND:

Airport Land Use Commission (ALUC) staff currently reviews a project's building intensity by using the 1) Building Code method and 2) Parking Code method, as outlined in the Countywide Plan. These approved methodologies are sufficient enough for staff to perform intensity calculations on most projects. However, in the case of Parcel Delivery Facilities, these two methodologies independently are not enough to calculate the project's intensity. The specific use of Parcel Delivery Facilities, which includes the use of indoor van parking spaces, loading spaces, and queuing/stacking spaces, in addition to the regular building floor area such as warehouse, manufacturing, office, etc., requires the use of a hybrid intensity methodology that is not provided in the Countywide Plan so staff can perform more realistic intensity calculations.

A discussion with the ALUC was held on March 11, 2021, where ALUC staff was directed to bring back a Policy for the Commission's consideration. This Policy outlines ALUC staff's recommended approach to reviewing Parcel Delivery Facilities as it relates to intensity calculation.

POLICY:

When a Parcel Delivery Facility project is submitted to ALUC, ALUC staff is authorized to consider using a "hybrid intensity methodology" to calculate the project's intensity when calculating intensity by the Building Code method and Parking Code method independently are insufficient and where the project's design involves the use of indoor van parking spaces, loading spaces, and/or queuing/stacking spaces. Specifically, the "hybrid intensity methodology" will use a combination of the Building Code method for such areas as normally defined by the Building Code, such as warehouse, office and manufacturing areas, as well as the Parking Code method, which will be applied to areas inside the building identified as indoor van delivery parking, loading, queuing and stacking.

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

STAFF REPORT

ADMINISTRATIVE ITEMS

5.1 Director's Approvals.

- A. During the period of March 16, 2021, through April 15, 2021, as authorized pursuant to Section 1.5.2(d) of the 2004 Riverside County Airport Land Use Compatibility Plan, ALUC Director Paul Rull reviewed two non-legislative cases within Zone E of the March Air Reserve Base/Inland Port Airport Influence Area.

ZAP1459MA21 (March Air Reserve Base/Inland Port Airport Zone E) pertains to County of Riverside Case No. PUP210002 (Public Use Permit), a proposal to construct a 53,466 square foot temple facility including a 4,566 square foot exhibition building, a 5,448 square foot prayer hall, a two-story 11,918 square foot auditorium, two 2,298 square foot meeting rooms, a 3,515 square foot facility office building, a 9,792 square foot Sunday School building, a 8,011 square foot dining hall, a 1,854 square foot library, and a 3,216 square foot ensuite building on 30.79 acres located southerly of Landin Lane, westerly of Cole Avenue, northerly of Markham Street, and easterly of Barton Street. The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, non-residential intensity is not restricted.

The elevation of Runway 14-32 at March Air Reserve Base/Inland Port Airport is approximately 1,535 feet above mean sea level (AMSL) at its northerly terminus. At a distance of 19,000 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with an elevation at top of roof exceeding 1,725 feet AMSL. The project site elevation is 1,684 feet AMSL, and the tallest proposed structure height is 40 feet, for a maximum top point elevation of 1,724 feet AMSL. Therefore, Federal Aviation Administration (FAA) obstruction evaluation review for height/elevation reasons is not required.

ALUC Director Paul Rull issued a determination of consistency for this project on March 24, 2021.

ZAP1460MA21 (March Air Reserve Base/Inland Port Airport Zone E) pertains to County of Riverside Case No. TPM37869 (Tentative Parcel Map), a proposal to construct a proposal to divide 4.82 gross acres located on the southwest corner of Leon Road and Bench Road into two residential lots. The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, residential density is not restricted.

Although the project is located within the March Air Reserve Base/Inland Port Airport Influence Area, the nearest runway is actually Runway 5-23 at Hemet Ryan Airport. The elevation of Runway 5-23 at Hemet Ryan Airport is approximately 1,499 feet above mean sea level (AMSL) at its southwesterly terminus. At a distance of 28,640 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with an elevation at top of roof exceeding 1,785 feet AMSL. The project site elevation is 1,714 feet AMSL. No building permits

for new structures are in process at this time, and review by the Federal Aviation Administration Obstruction Evaluation Services (FAA OES) is not a prerequisite to land division. Therefore, FAA OES review for height/elevation reasons was not required.

ALUC Director Paul Rull issued a determination of consistency for this project on April 1, 2021.

- B. Additionally, as authorized pursuant to ALUC Resolution No. 2015-01, as extended by Resolution Nos. 2016-02 and 2018-02, ALUC Director Paul Rull reviewed one legislative case within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area and issued determination of consistency.

ZAP1463MA21 (March Air Reserve Base/Inland Port Airport, Zone E) pertains to City of Moreno Valley Case Nos. PEN20-0095 (General Plan Amendment), PEN20-0096 (Change of Zone), PEN21-0066 (Conditional Use Permit), a proposal to develop 108 detached condominium units on 17.3 acres within a 32.7 acre parcel located easterly of Gernert Road, northerly of Jennings Court, westerly of Hilmer Court, and southerly of the Box Springs Mountain Reserve Park. The applicant also proposes to amend 17.3 acres of the site's General Plan land use designation from Residential: Max 2 DU/AC to Residential: Max 10 DU/AC, and to amend the remaining 15.4 acres of the site's General Plan land use designation from Hillside Residential to Open Space, as well as a change the zoning of 17.3 acres from R2-2DU/AC to RS10-10DU/AC, and a change to the zoning of the remaining 15.4 acre portion of the site from Hillside Residential to Open Space. The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, residential density and non-residential intensity are not restricted.

The elevation of Runway 14-32 at March Air Reserve Base/Inland Port Airport is approximately 1,535 feet above mean sea level (AMSL) at its northerly terminus. At a distance of 23,300 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with an elevation at top of roof exceeding 1,768 feet AMSL. The project site elevation is 1,680 feet AMSL, and the tallest proposed structure is 35 feet, for a maximum top point elevation of 1,715 feet AMSL. There are no proposed changes to the existing building height. Therefore, Federal Aviation Administration (FAA) obstruction evaluation review for height/elevation reasons is not required.

ALUC Director Paul Rull issued a determination of consistency for this project on April 14, 2021.

- C. Additionally, as authorized pursuant to ALUC Resolution No. 2020-02, ALUC Director Paul Rull reviewed one non-legislative case within Airport Compatibility Zone D of the French Valley Airport Influence Area and issued determination of consistency.

ZAP1110FV21 (French Valley Airport Influence Area Zone D) pertains to County of Riverside Case No. PP26344 (Plot Plan Amendment), a proposal to revise the internal floor plan for a 34,000 square foot Fitness Center (Building #11) as part of a 132,568 square foot retail commercial center on 21.16 acres which was previously found consistent under ZAP1087FV19, located on the northeast corner of Thompson Road and Highway 79 Winchester Road. The proposed changes to the internal floor area of the 34,000 square foot fitness center building includes: 16,748 square feet of exercise room area, 1,020 square feet of office area, 105 square feet of reception/lounge area, 85 square feet of retail area, 2,146 square feet of daycare area, 1,308 square feet of locker room area, 1,916 square feet of pool area, 370 square feet of mechanical equipment room area, and 626 square feet of storage area. The proposed revisions result in a total occupancy

of 484 people, which results in an average intensity of 132 people per acre, and a single acre intensity of 484 people, both of which are consistent with Compatibility Zone D average and single acre criterion of 150 and 518 respectively. (Note, a 518 occupancy limit was based on the single-acre risk reduction measures that was granted to the original project for this building, warranting a 15% bonus to the normal single acre criterion, increasing it from 450 people to 518 people). There are no changes to the footprint size of the building. The proposed revisions does not result in a change of occupancy (484 people) and intensity (132 people average acre and 518 people single acre) from the original project ZAP1087FV19.

ALUC Director Paul Rull issued a determination of consistency for this project on April 7, 2021.

- 5.2** Update March Air Reserve Base Compatibility Use Study (CUS)
Presentation by Project Director Simon Housman or his designee.

AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY



March 24, 2021

Mr. Timothy Wheeler, Project Planner
Riverside County Planning Division
4080 Lemon Street, 12th Floor
Riverside CA 92501

CHAIR
Russell Betts
Desert Hot Springs

VICE CHAIR
Steven Stewart
Palm Springs

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –
DIRECTOR'S DETERMINATION**

COMMISSIONERS

Arthur Butler
Riverside

John Lyon
Riverside

Steve Manos
Lake Elsinore

Richard Stewart
Moreno Valley

Gary Youmans
Temecula

STAFF

Director
Paul Rull

Simon A. Housman
Paul Rull
Barbara Santos

File No.: ZAP1459MA21
Related File No.: PUP210002 (Public Use Permit)
APN: 266-320-025
Airport Zone: Compatibility Zone E

Dear Mr. Wheeler:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed County of Riverside Case No. PUP 210002 (Public Use Permit), a proposal to construct a 53,466 square foot temple facility including a 4,566 square foot exhibition building, a 5,448 square foot prayer hall, a two-story 11,918 square foot auditorium, two 2,298 square foot meeting rooms, a 3,515 square foot facility office building, a 9,792 square foot Sunday School building, a 8,011 square foot dining hall, a 1,854 square foot library, and a 3,216 square foot ensuite building on 30.79 acres located southerly of Landin Lane, westerly of Cole Avenue, northerly of Markham Street, and easterly of Barton Street.

The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, non-residential intensity is not restricted.

The elevation of Runway 14-32 at March Air Reserve Base/Inland Port Airport is approximately 1,535 feet above mean sea level (AMSL) at its northerly terminus. At a distance of 19,000 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with an elevation at top of roof exceeding 1,725 feet AMSL. The project site elevation is 1,684 feet AMSL, and the tallest proposed structure height is 40 feet, for a maximum top point elevation of 1,724 feet AMSL. Therefore, Federal Aviation Administration (FAA) obstruction evaluation review for height/elevation reasons is not required.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, provided that the County of Riverside applies the following recommended conditions:

County Administrative Center
4080 Lemon St., 14th Floor
Riverside, CA 92501
(951) 955-6132

www.aluc.org

AIRPORT LAND USE COMMISSION

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site.
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Hazards to flight.
3. The attached notice shall be provided to all prospective purchasers of the property and tenants of the building.
4. The maximum height of the building, including all roof-mounted equipment, if any, shall be limited to 40 feet, and the maximum top point elevation shall not exceed 1,725 feet above mean sea level unless a "Determination of No Hazard to Air Navigation" letter authorizing a higher top point elevation has been issued by the Federal Aviation Administration Obstruction Evaluation Service.

If you have any questions, please feel free to contact me at (951) 955-6893.

Sincerely,

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Paul Rull, ALUC Director

AIRPORT LAND USE COMMISSION

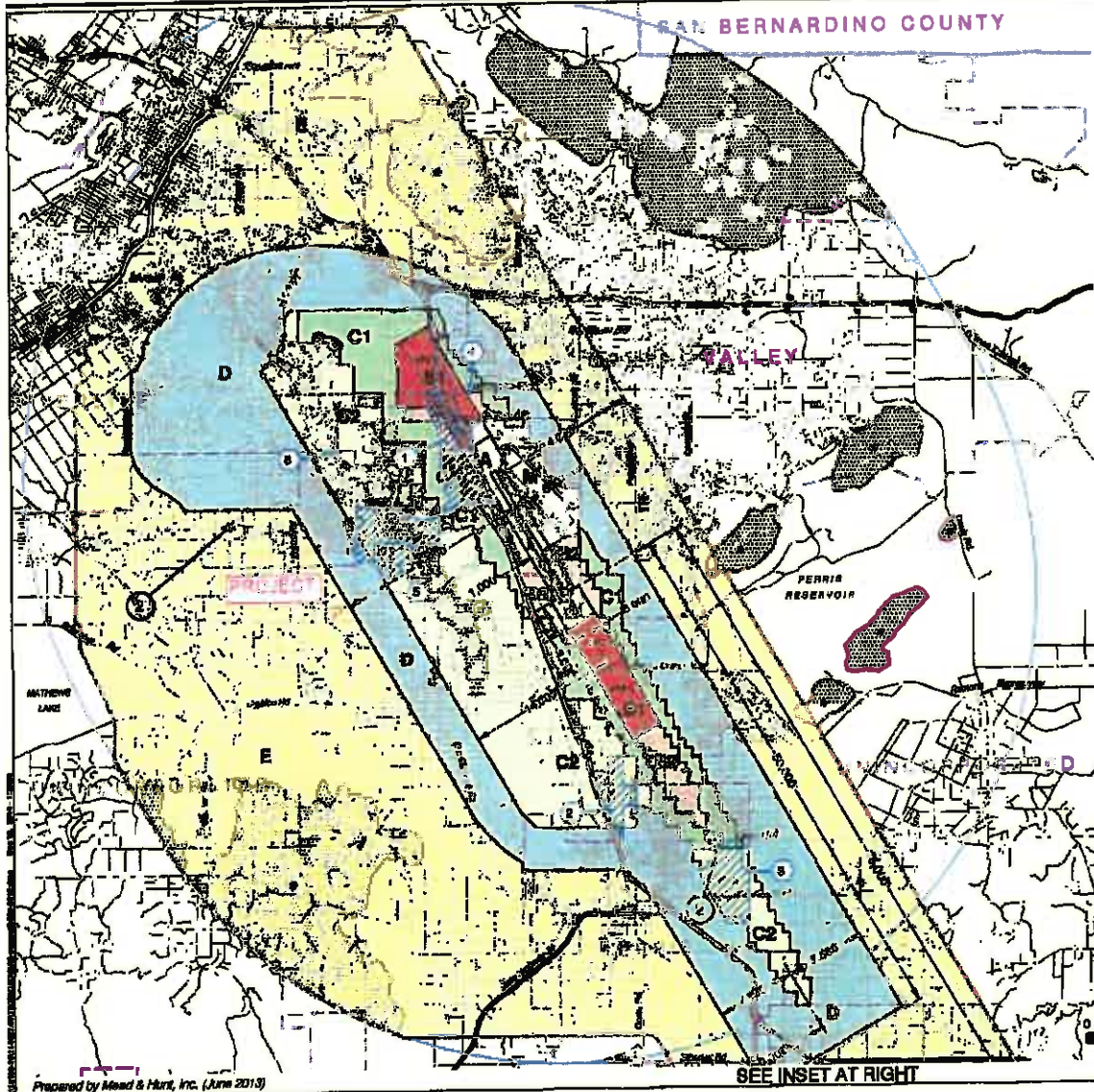
Attachments: Notice of Airport in Vicinity

**cc: Robert Ritner (applicant)
Berkin Construction c/o Richard Chou (representative)
Chong Huo Tong Moral Association USA, Inc. (property owner)
Gary Gosliga, Airport Manager, March Inland Port Airport Authority
Doug Waters, Deputy Base Civil Engineer, March Air Reserve Base
ALUC Case File**

Y:\AIRPORT CASE FILES\March\ZAP1459MA21\ZAP1459MA21.LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



LEGEND

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

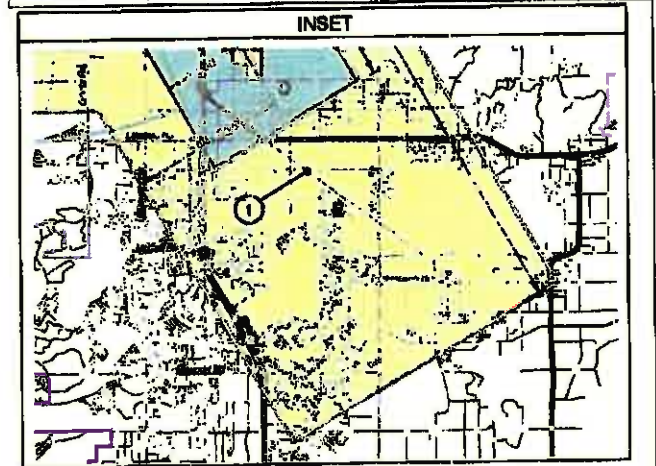
Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)

- ① March JPA: March Business Center/Meridian
- ② Perris: Harvest Landing
- ③ Perris: Park West
- ④ Moreno Valley: Affordable Housing
- ⑤ March JPA: Ben Clark Training Center
- ⑥ Riverside: Ridge Crest Subdivision

① Point at which aircraft on Runway 02 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,835 feet MSL.

② Point at which departing aircraft typically reach 3,000 feet above runway end.



Riverside County
 Airport Land Use Commission
 March Air Reserve Base / Inland Port Airport
 Land Use Compatibility Plan
 (Adopted November 13, 2014)

Note:
 All dimensions are measured from
 runway ends and centerlines.

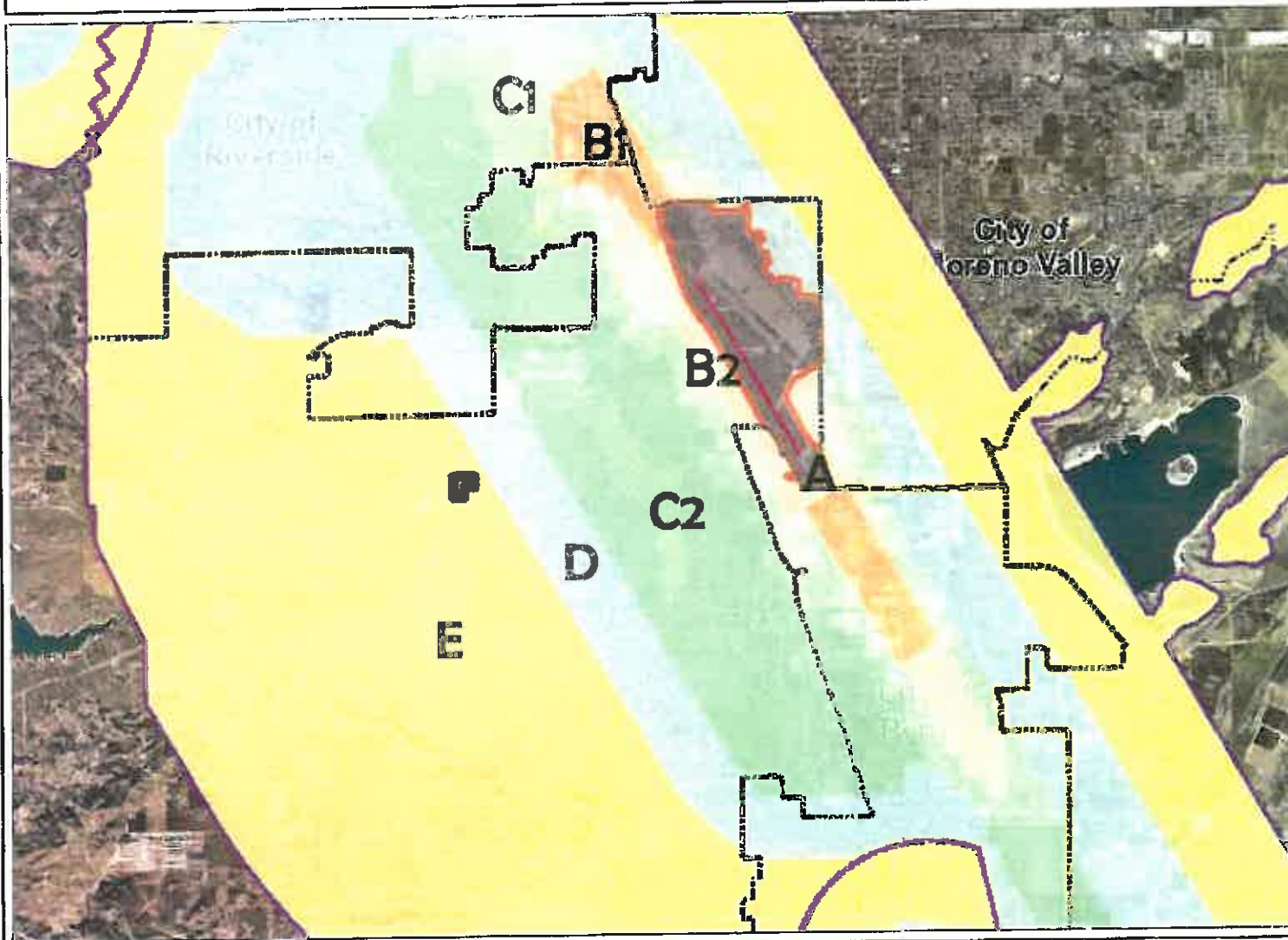


Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map MA-1
Compatibility Map
 March Air Reserve Base / Inland Port Airport

Map My County Map



- Legend**
- Runways
 - Airports
 - Airport Influence Areas
 - Airport Compatibility Zones**
 - OTHER COMPATIBILITY ZONE
 - A
 - A-EXC1
 - B1
 - B1-APZ I
 - B1-APZ I-EXC1
 - B1-APZ II
 - B1-APZ II-EXC1
 - B1-EXC1
 - B2
 - B2-EXC1
 - C
 - C1
 - C1-EXC1
 - C1-EXC3
 - C1-EXC4
 - C1-HIGHT
 - C2
 - C2-EXC1
 - C2-EXC2
 - C2-EXC3
 - C2-EXC5
 - C2-EXC6



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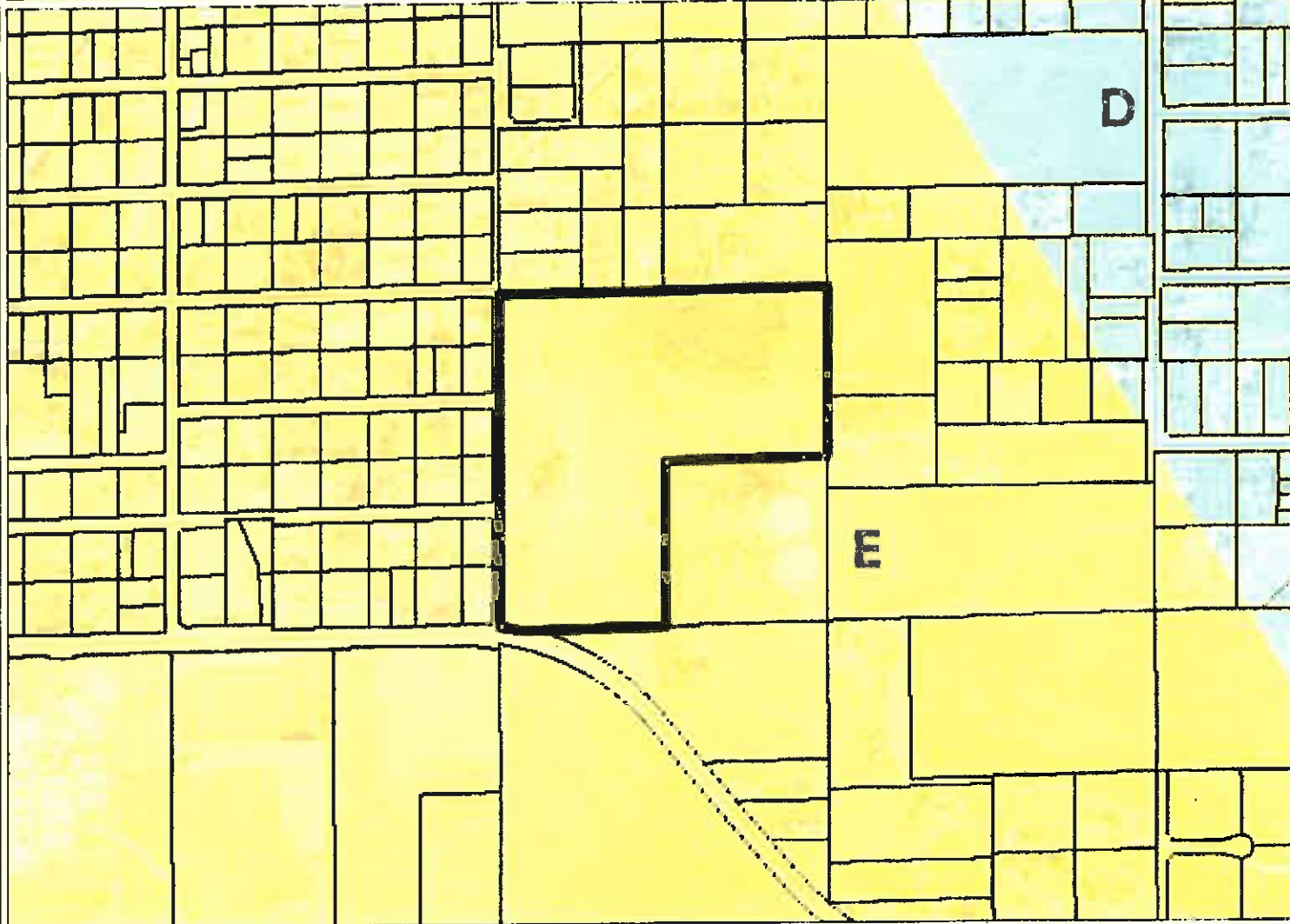
Notes



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Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-NIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC6

Notes



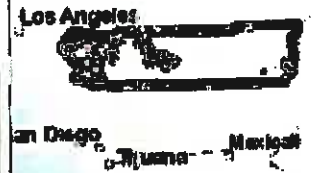
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0 752 1,505 Feet

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Map My County Map



Legend

-  Parcels
-  County Centerlines
-  Blueline Streams
-  City Areas
-  World Street Map

Notes



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Map My County Map



Legend

- Blue line Streams
- City Areas
- World Street Map

Notes



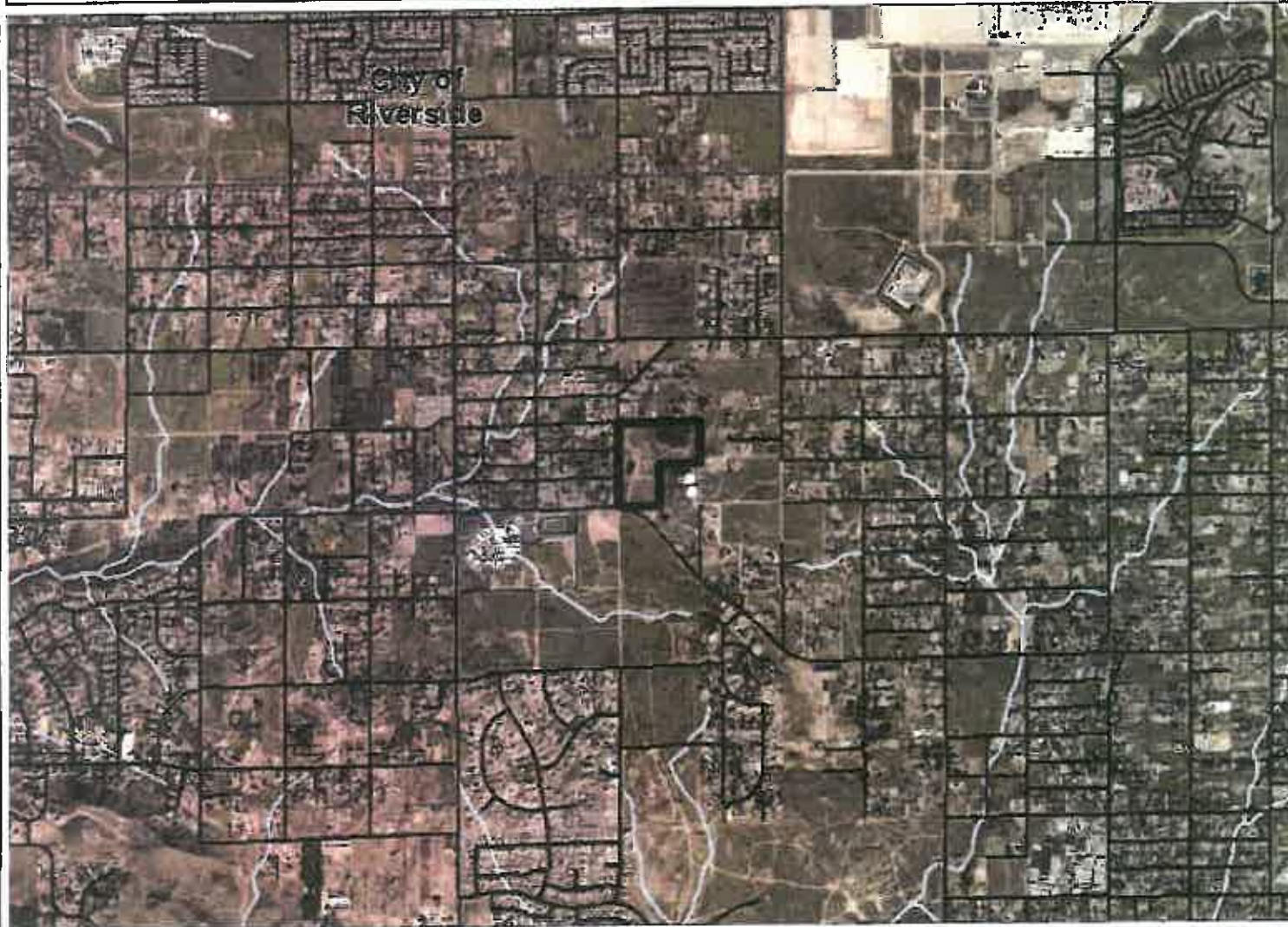
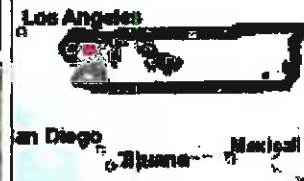
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Map My County Map



Legend

- County Centerlines
- Blue line Streams
- City Areas
- World Street Map

Notes



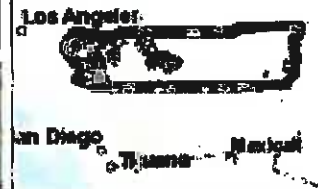
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Map My County Map



Legend

-  Parcels
-  County Centerlines
-  BlueLine Streams
-  City Areas
-  World Street Map



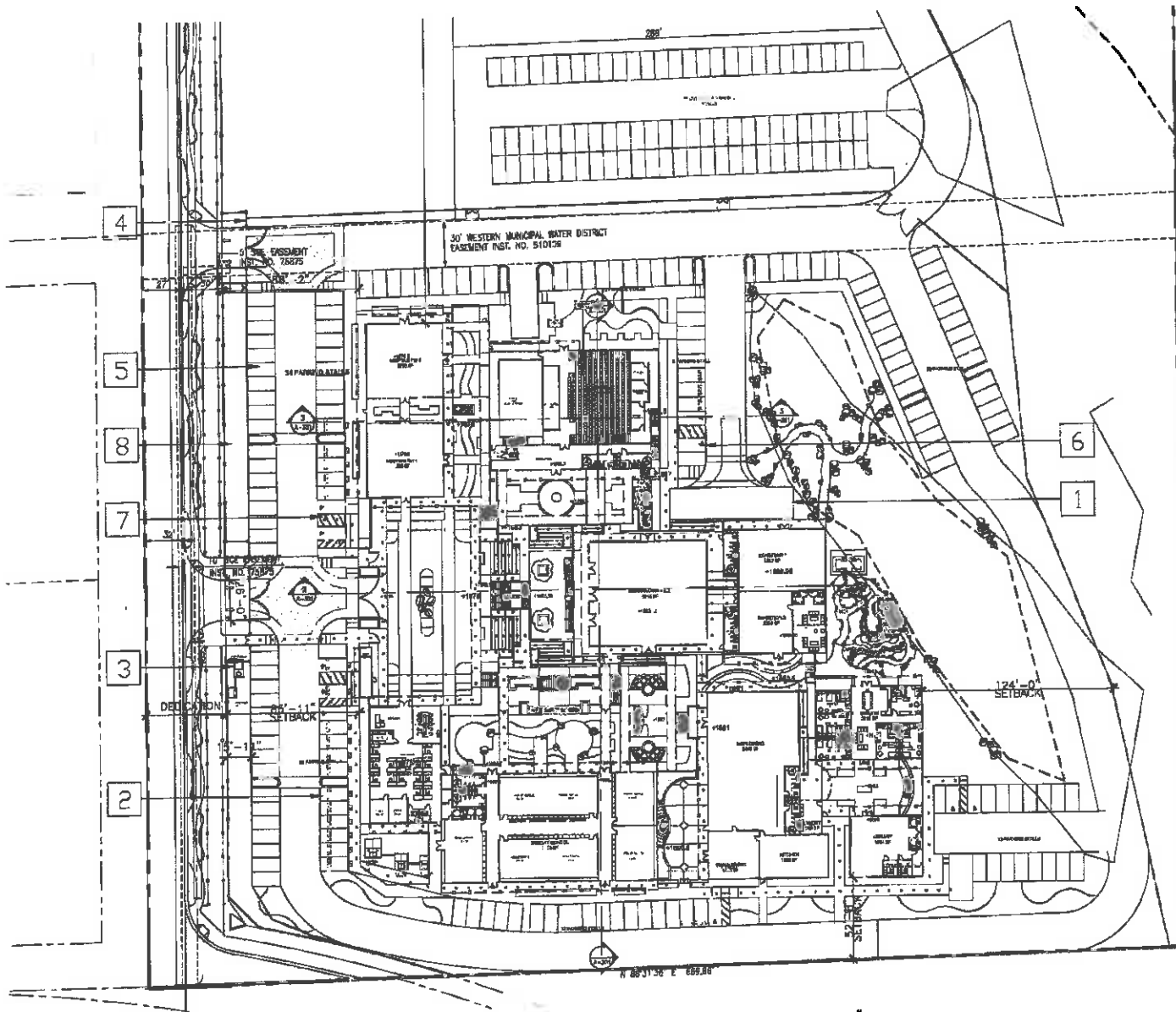
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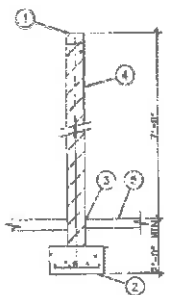
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Notes



① ENLARGED SITE PLAN
SCALE 1/8\"/>

- KEY NOTES & LEGEND**
- ① FORE TRUCK TURNAROUND
 - ② DESIGNATED PARKING (LOW-EMITTING, FLD, EFFICIENT AND CARPOOL/VANPOOL VEHICLES)
 - ③ TRANSIT DRIVER
 - ④ 7'-0" CMU FENCE WALL
 - ⑤ STANDARD PARKING 8' X 12' X 20'
 - ⑥ HANDICAP PARKING 8' X 12' X 20'
 - ⑦ ADA VAN PARKING 12' X 20'
 - ⑧ LANDSCAPE



- 1. SOLID CMU CAP
- 2. CONTINUED 1 - 1"ING
- 3. SCREW-DRIVER ALL DEFS BELOW GRADE
- 4. 6" X 8" X 11" CMU - SURFACE COLOR MATCH BLINDS
- 5. FINISHED GRADE/FLOOR

② CMU FENCE WALL
SCALE 3/4\"/>

- SITE PLAN LEGEND**
- PROPERTY LINE
 - SET BACK
 - ACCESSIBLE PATH OF TRAVEL

C D A
 Creative Design Associates, Inc.
 17330 E. 80th Street
 City of Industry, CA 91748
 (909) 811-9107
 Fax: (909) 811-9101

Architects - Interior Design
 Planning
 Supply: ©/Creative Design Associates, Inc.

Project:
**I KUAN TAO
 TEMPLE**
 COUNTY OF APPROVAL: 0008 010
 1/15/2010
 Client:
IBUKUN CONSTRUCTION
 1000 GRAND AVE. S.W. 100
 SUITE 200 WALKER
 MI 48090



**NOT FOR
 CONSTRUCTION
 OR
 RECORDING**

Drawn by: **ENLARGED
 SITE PLAN & FENCE
 WALL DETAILS**

CDM Planning No. 2014
 DATE: 2009.10.20
 PROJECT: 00
 DRAWN BY: CDM
 CHECKED BY: CDM
 PROJECT NO.
 SHEET NO.
 DATE: 2010

AS-102



C D A

Construction Design Architecture, Inc.
 17425 East Fremont Street
 City of Industry, CA 91748
 Tel: 626-211-8000
 Fax: 626-211-8181

Architects - Interior Design
 Planning
 Copyright © 2014 Construction Design Architecture, Inc.

Project:
**I KUAN TAO
 TEMPLE**

Contractor selected by SCORING
 BOARD OF CA

Contract:
BROWN CONSTRUCTION
 10400 WARDEN AVE. #100
 WEST GARDENA, CA 90747
 TEL: 562-420-8800

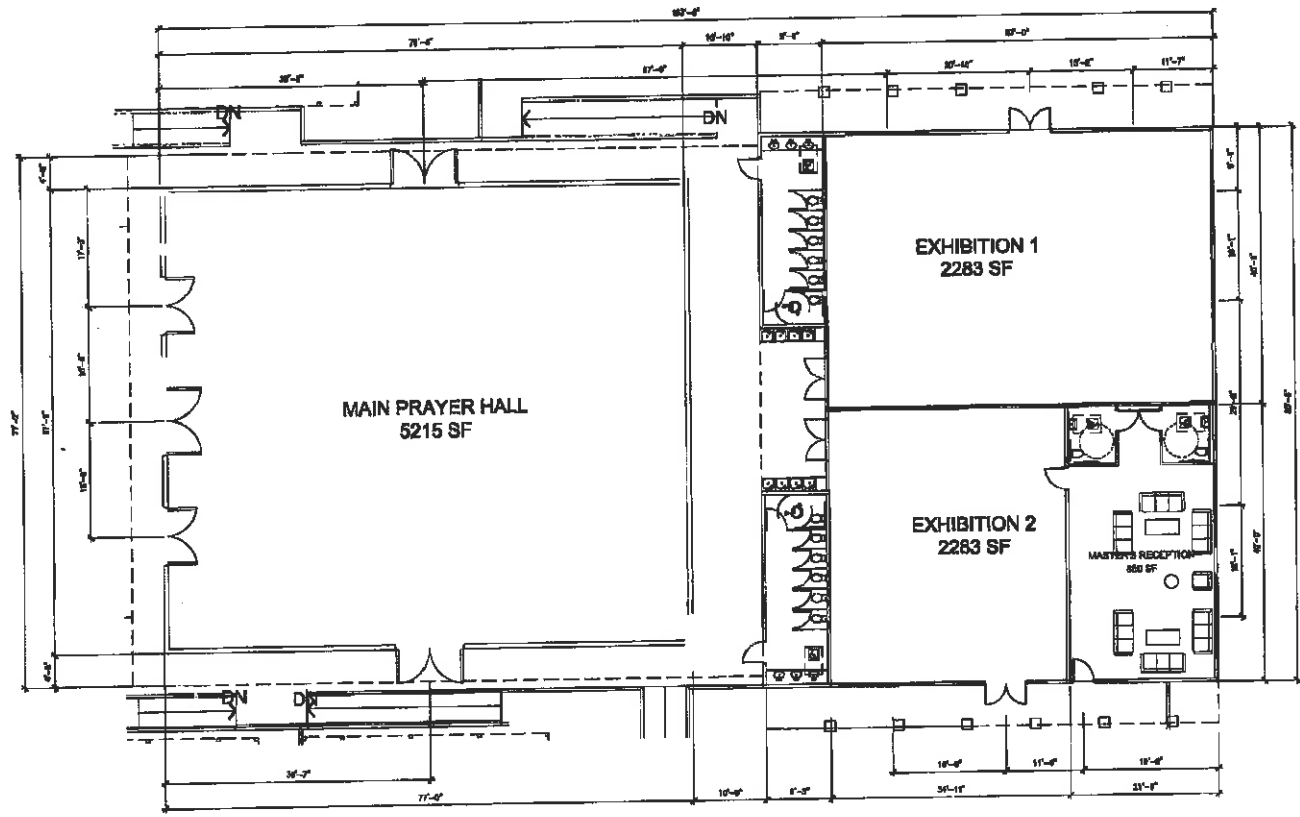


**NOT FOR
 CONSTRUCTION
 OR
 RECORDING**

Working Title:
**BLDG 1
 1ST FLOOR PLA.**

CDR Project No. 107
 Date: 08/14/14
 Phase: SD
 Checked By: CDR
 Drawn By: CDR
 Project: I
 Reference:
 No. 13742

Sheet No.
A-101



1 1ST FLOOR PLAN
 SCALE: 1/8" = 1'-0"



C D A
 Creative Design Associates, Inc.
 17000 E. Rowland Street
 City of Industry, CA 91745
 626-872-0107
 626-872-0188
 ARCHITECTS - INTERIOR DESIGN
 PLANNING
 Designer: Dale Chubb Design Associates, Inc.

Project:
**I KUAN TAO
 TEMPLE**
 OWNER/CLIENT: CHEN AND
 ASSOCIATES, CA
 01/04/11
BRUSH CONSTRUCTION
 518 BRUSH CANYON RD
 BEVERLY HILLS, CA
 90210-1291

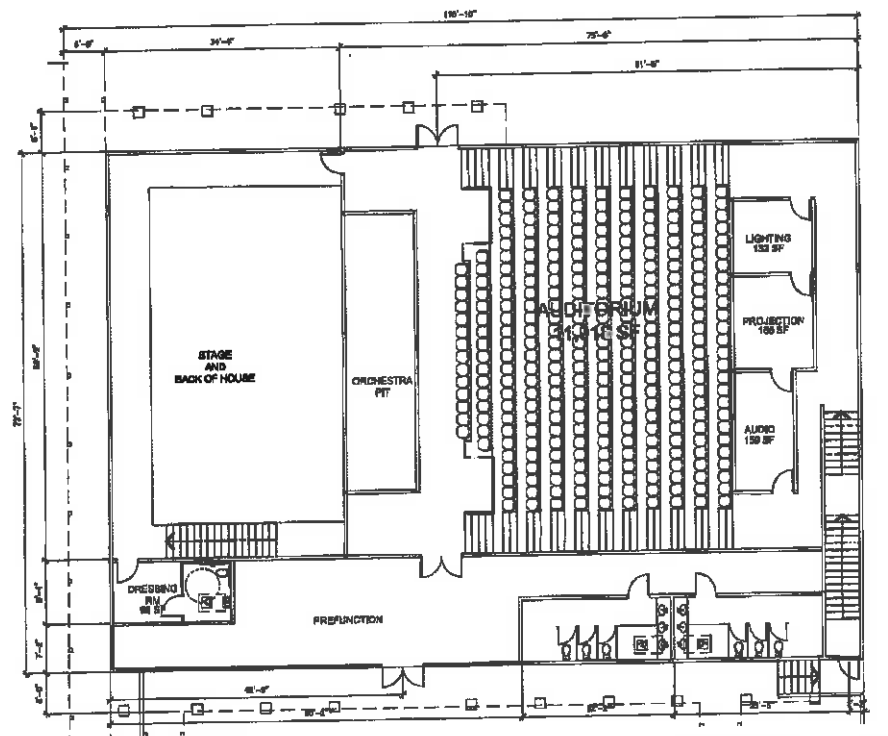


**NOT FOR
 CONSTRUCTION
 OR
 RECORDING**

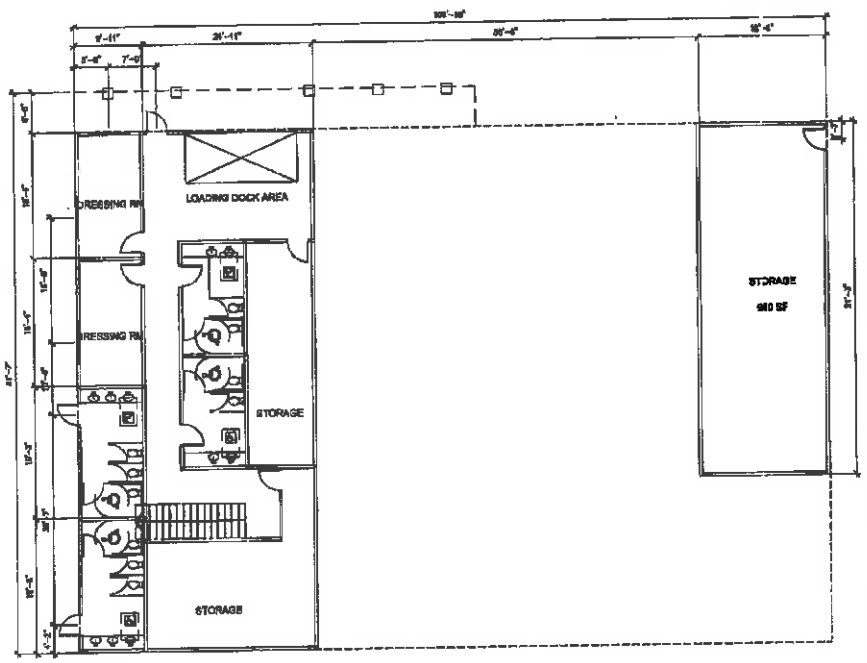
Drawing Title
**BLDG 2
 1ST FLR
 & 2ND FLR PLAN**

CSA Project No. 2011
 Date: 08/16/2011
 Project No.
 Checked By: CDM
 Drawn By: CDM
 Project No.
 Reference
 Date: 1/11/11

Drawing No.
A-102



2 2ND FLOOR PLAN
 SCALE 1/8" = 1'-0"



1 1ST FLOOR PLAN
 SCALE 1/8" = 1'-0"



C D
 Creative Design Associates
 1200 East Riverfront Street
 Cary, North Carolina, NC 27513
 P: 919.233.1100
 F: 919.233.1190
 Architects - Interior Design
 10/14/19
 Graphic: 9/19 Creative Design Associates

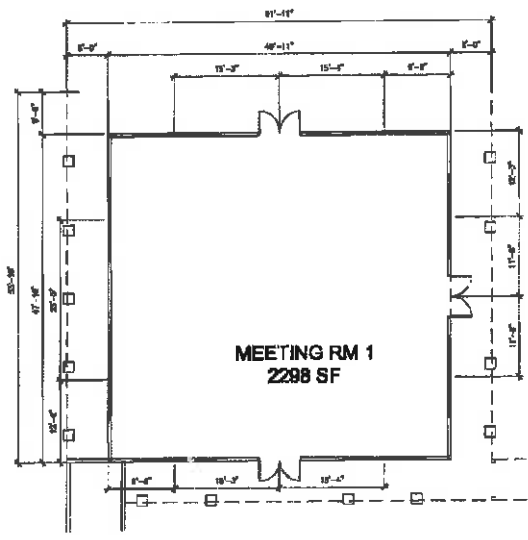
Project:
I KUAN TAO TEMPLE
 1000 S. UNIVERSITY AVE. CAMPUS
 SUITE 200, CHAS. ON
 Drawn:
BRUNN CONSTRUCT
 100 S. UNIVERSITY AVE. SUITE 200
 CHAS. ON, NC 27513
 DATE: 10/14/19

Floor:

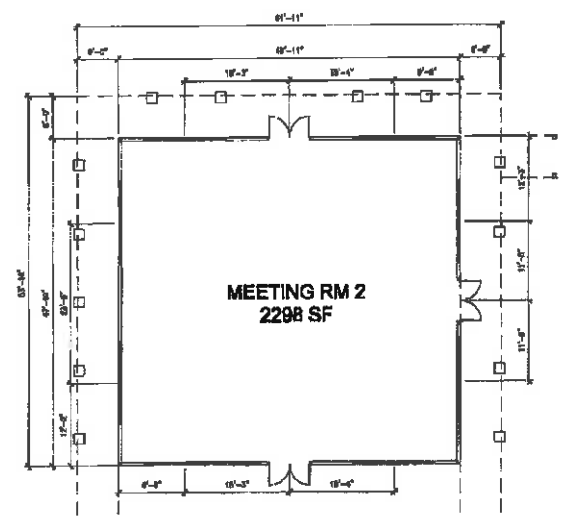


NOT FOR CONSTRUCTION OR RECORDING

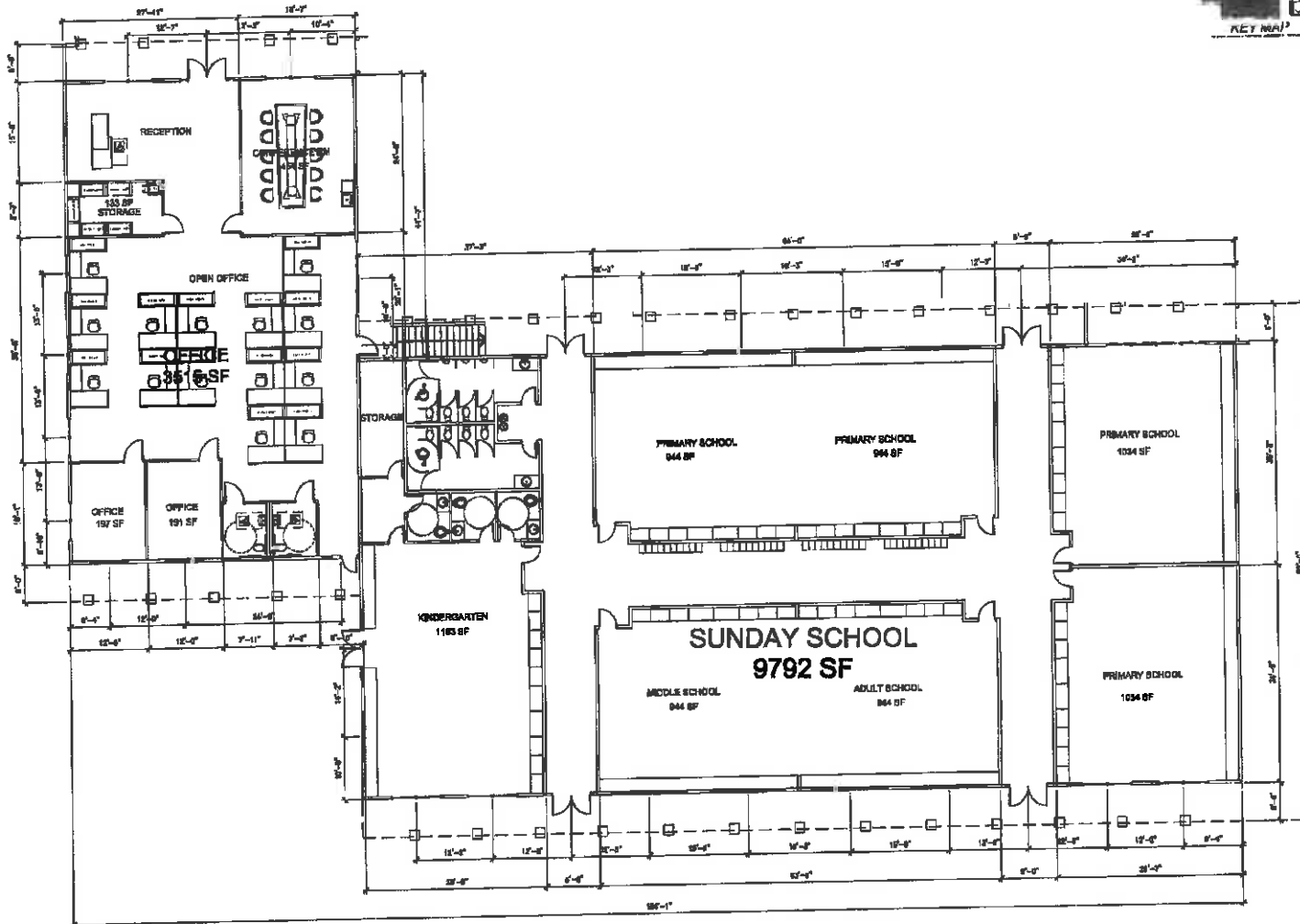
Drawing Title:
BLDG 3 & BLDG 4 FLOOR PLAN
 CDA Project No.: 1919
 Date: 10/14/19
 Name: BR
 Designer: BR
 Checker: BR
 Project No.: 1919
 Scale: 1/8" = 1'-0"
 App'd: BR



② **BLDG 4 MEETING 1 RM FLOOR PLAN**
 SCALE: 1/8" = 1'-0"

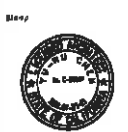


① **BLDG 3 MEETING 2 RM FLOOR PLAN**
 SCALE: 1/8" = 1'-0"



C D A
 Creative Design Architecture, Inc.
 17020 Boral Boulevard Street
 City of Pasadena, CA 91761
 626 792 8181
 FAX 626 792 8181
 Architecture - Interior Design
 Planning
 Concept by Dennis Deryn Associates, Inc.

Project
**I KUAN TAO
 TEMPLE**
 COMPANY: CHEN HUA & CO., INC.
 ARCHITECT: CHEN
 CLIENT:
BURKH CONSTRUCTION
 1744 GARDEN STREET
 BEVERLY HILLS, CA 90210
 310 784 3222



**NOT FOR
 CONSTRUCTION
 OR
 RECORDING**

Assembly Title
**BLDG 5
 1ST FLOOR PLN.**

CDR Project No. 004
 00000 00000000
 Pages: 30
 Checked By: CDA
 Drawn By: CDA
 Plot Date: 10/10/00
 Reference:
 Rev. 01/10

1 BLDG 5 SUNDAY SCHOOL AND ADMINISTRATION FLOOR PLAN
 SCALE: 1/8" = 1'-0"



C D A
 Creative Design Associates, Inc.
 17520 West Montross Street
 City of Monterey, CA 93928
 (408) 932-2885
 Fax: (408) 932-2887
 www.cda.com
 Architect/Designer - Interior Design
 Plans Only
 Copyright © by Creative Design Associates, Inc.

PROJECT:
I KUAN TAO TEMPLE
 COMPANY: HUNTSVILLE ARCHITECTURE
 ARCHITECT
 CLIENT:
BERYON CONSTRUCTION
 1525 W. BIRCH AVE. SUITE 103
 CHICO, CA 95926
 (530) 891-1111
 www.beryon.com

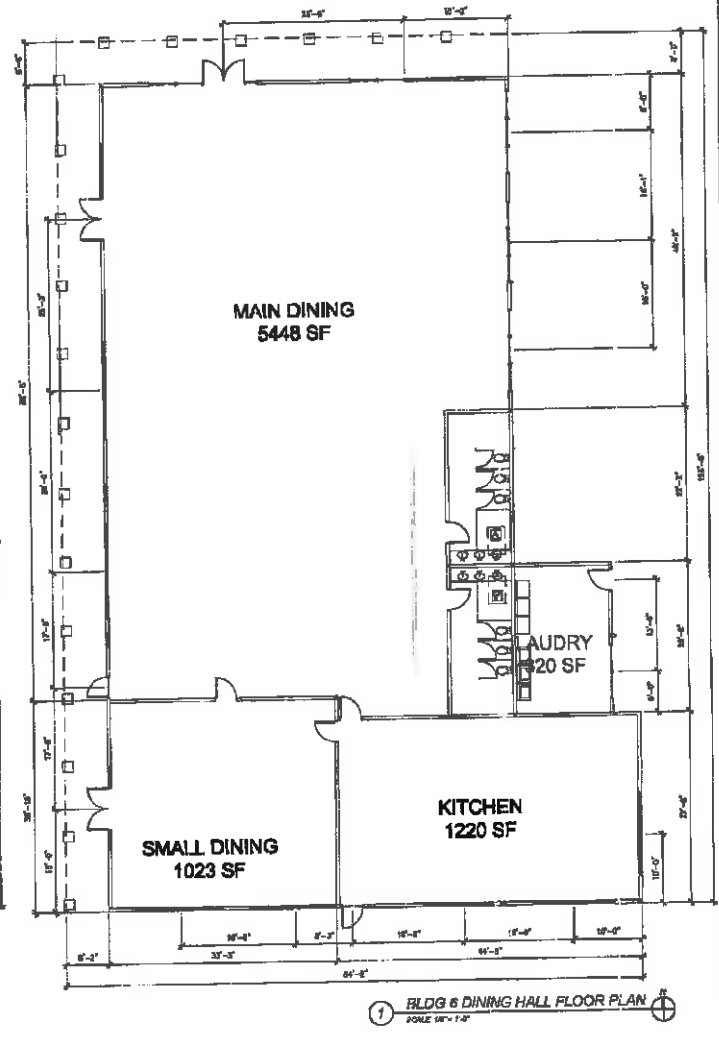
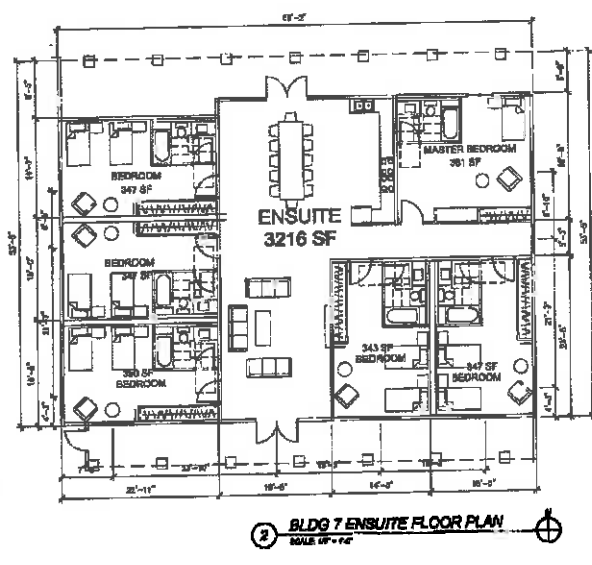
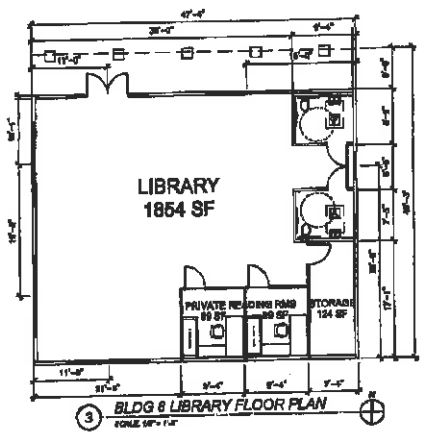
22-09

NOT FOR CONSTRUCTION OR RECORDING

Drawing Title:
BLDG 6, 7, & 8 1ST FLOOR PLANS

CSA Project No. 224
 Date: 02/16/2009
 Project ID:
 Checked By: CDA
 Drawn By: C.A.
 Project in Reference
 See 2004

Sheet No.
A-105



C D A

Charles Design Associates, Inc.
1722 West Beach Blvd.
City of Indio, CA 92540
951.437.1007
www.charlesdesign.com

Architect - exterior design
Planning
Copyright © Charles Design Associates, Inc.

Project:
**I KUAN TAO
TEMPLE**

OWNER: COUNTY OF INDIO
INDIO, CA

Client:
BIRCH CONSTRUCTION
1888 WEST JUNG DRIVE
INDIO, CA 92517
P. 951-739



② **SOUTH ELEVATION**
SCALE: 1/8" = 1'-0"



① **STREETWEST ELEVATION**
SCALE: 1/8" = 1'-0"



**NOT FOR
CONSTRUCTION
OR
RECORDING**

Project:
**PROJECT.
ELEVATIONS**

CDM Project No. 100
Date: 08/16/2008
Phase: 02
Created By: CDM
Drawn By: CDM
Project: "A"
Subject:
Rev. 001

Sheet No.
A-201

C D A

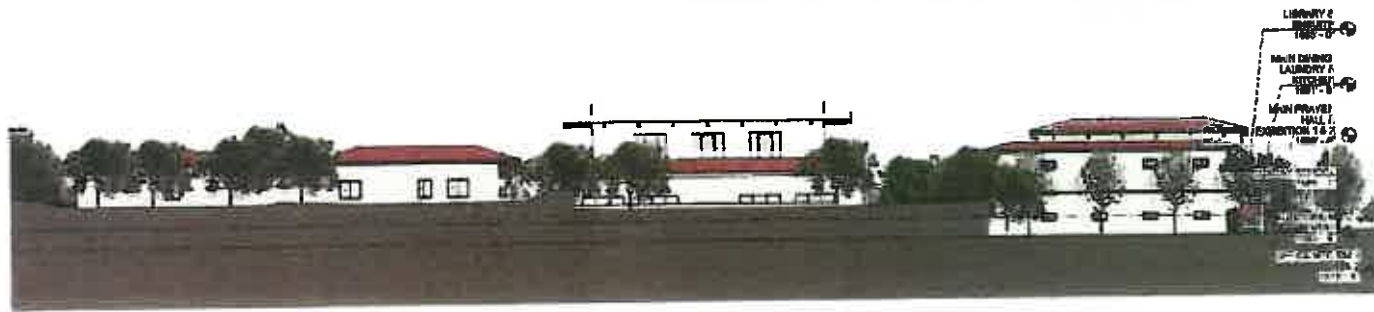
Conceptual Design Associates, Inc.
17020 Wood Boulevard Street
City of Los Angeles, CA 91048
(818) 712-1100
PROJECT NO. 2101

Architectural - Interior Design
Planning
Conceptual Design Associates, Inc.

Project:
**I KUAN TAO
TEMPLE**

OWNER: UNIVERSITY & COLLEGE
BUNNICK, CA

Client:
BRINK CONSTRUCTION
2024 BRINK AVE SUITE 222
WEST GARDEN, CA 91307
714-340-8888



② **EAST ELEVATION**
SCALE: 1/8" = 1'-0"



③ **SOUTH ELEVATION**
SCALE: 1/8" = 1'-0"



**NOT FOR
CONSTRUCTION
OR
RECORDING**

Bearing Title:
**PROJECT
ELEVATIONS**

CDA Project No. 2101
Date: 02/16/2023
Page: 03
Checked By: CDA
Drawn By: CDA
Project No. 2101
Revisions
Rev. 01.00

Drawing No.
A-202



12 BLDG 4 MEETING RM 1 WEST ELEVATION
SCALE 1/8" = 1'-0"



11 BLDG 4 MEETING RM 1 SOUTH ELEVATION
SCALE 1/8" = 1'-0"



10 BLDG 4 MEETING RM 1 NORTH ELEVATION
SCALE 1/8" = 1'-0"



9 BLDG 4 MEETING RM 1 EAST ELEVATION
SCALE 1/8" = 1'-0"



8 BLDG 3 MEETING RM 2 WEST ELEVATION
SCALE 1/8" = 1'-0"



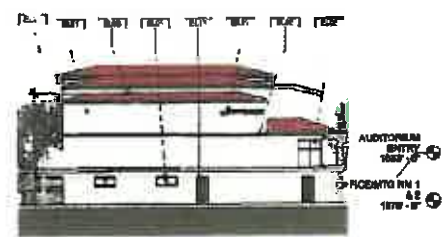
7 BLDG 3 MEETING RM 2 SOUTH ELEVATION
SCALE 1/8" = 1'-0"



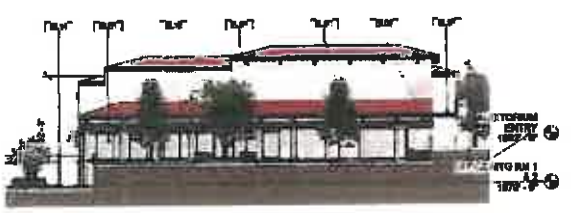
8 BLDG 3 MEETING RM 2 NORTH ELEVATION
SCALE 1/8" = 1'-0"



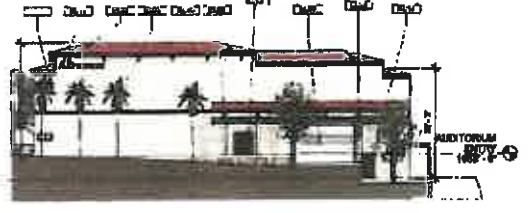
6 BLDG 3 MEETING RM 2 EAST ELEVATION
SCALE 1/8" = 1'-0"



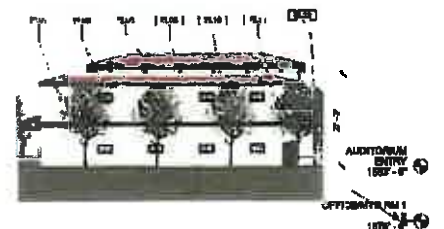
4 BLDG 2 AUDITORIUM WEST ELEVATION
SCALE 1/8" = 1'-0"



3 BLDG 2 AUDITORIUM SOUTH ELEVATION
SCALE 1/8" = 1'-0"



2 BLDG 2 AUDITORIUM NORTH ELEVATION
SCALE 1/8" = 1'-0"



1 BLDG 2 AUDITORIUM EAST ELEVATION
SCALE 1/8" = 1'-0"

EXTERIOR FINISH SCHEDULE

- 1101 STUCCO/PAINT, FRESH SAND, SANDRA, COLOR: P. FAIRBURY WHITE OR BURL
- 1102 STUCCO/PAINT, FRESH SAND, SANDRA, COLOR: P. SEA HAVEN WHITE OR BURL
- 1103 METAL WALL/ROOF PANEL: ASP. STAIN SLURRY/PAN, FINISH: SILKY JUST OR BURL
- 1104 WOOD VENEER: CUSTOMER SPEC. 1/2\"/>
- 1105 STUCCO/PAINT: CH. A.S. ALUMINUM/PAINT: CH. CO-INTERLAP 4 SHARP BEAM SYSTEM, COLOR: RED DARK BROWN VENEER/PAINT: PANE GRASS GLAZING GLASS/PAINT: BUREAU - IMPERMEABLE AS ON TO SURFACE - CONSTRUCTION OR BURL
- 1106 1\"/>
- 1107 WOOD VENEER: BUREAU APL/PAINT: ALUMINUM VENEER/PAINT: COLOR: BROWN, 100% VENEER, NO SHIP OR PAINT
- 1108 INSULATED CONCRETE BLOCKS: 8\"/>
- 1109 METAL CASES: PAINT: PRIMER: IMPERMEABLE: STAINLESS STEEL, SURFACE: ALUMINUM, STAINLESS STEEL COLUMN: W/ALUMINUM TOP PAINT, IMPERMEABLE: CONCRETE OR PAINT
- 1110 SCHEDULE FOR SEPARATE PART
- 1111 CONCRETE COLUMN
- 1112 DOOR
- 1113 METAL COLUMN: PAINTED TO COLOR: METAL: BURN BURNING: DETAIL: FORM OR BURL
- 1114 INSULATED CONCRETE BLOCKS: 8\"/>
- 1115 MOLDING: PAINTED TO COLOR: WHITE: CONCRETE: IMPERMEABLE: PAINT: OR BURL

C D A

Brady Bragg Associates, Inc.
1155 East Street Drive
P.O. Box 1000 - CA 91711
909-912-1141
Fax: 909-912-1141

Architects - Interior Design
Firm No.
Copyright © by Designer/Designer/Designer, Inc.

Project:
I KUAN TAO TEMPLE

Contract: ARCHITECT / CIVIL ENGINEER / INTERIOR DESIGNER

Drawn:
ARMY CONSTRUCTION
101 GEN. L. ST. #10
WESTON, VA 22187
703-338-2000



NOT FOR CONSTRUCTION OR RECORDING

Drawing Title:
BLDG. ELEVATIONS

SEA Project No. 094
Date: 08/14/09
Phase: 02
Sheet No.: 004
Project: I. Kuan Tao Temple
Reference: Rev: 000

EXTERIOR FINISH SCHEDULE

- 0201 STUCCO/PLASTER, WHITE SANTA BARBARA, COLOR F STAINLESS STEEL OR EQUAL
- 0202 STUCCO/PLASTER, PAINTED SANTA BARBARA, COLOR F BROWN/OLIVE WHITE OR EQUAL
- 0203 METAL WALL/ROOF PANELS SPAN SLAB/SHEATH, FINISH/PAINT AS NOTED
- 0204 STAINLESS STEEL CLADDING SYSTEM, 1/2" P/2" FINISH, COLOR PLATINUM OR EQUAL
- 0205 WOOD JOIST PAINTED TO COLOR MATCH EXISTING EXTERIOR DETAIL, 1" BROWN OR EQUAL
- 0206 STONE/PAINT ON-ALL ALUMINUM/STAINLESS STEEL/CLADDING SYSTEM, COLOR AND BARS BRONZE WITH PAINT CLEAR GLASSING/CLADDING SYSTEM - SUPPLEMENTAL AS SHOWN - CONFORM WITH CODES
- 0207 CLADDING SPAN PANEL/FINISH/PAINT, 1/2" BROWN, COLOR COOL BROWN WHITE OR EQUAL
- 0208 WOOD JOIST PAINTED BURNING OAK/PAINT CLADDING SYSTEM, COLOR BROWN, LOW-V GLAZING, NO GRILL OR EQUAL
- 0209 WOOD JOIST PAINTED BURNING OAK/PAINT CLADDING SYSTEM, COLOR BROWN, LOW-V GLAZING, NO GRILL OR EQUAL
- 0210 METAL CHIMNEY/PAINTED FINISH/CLADDING SYSTEM, COLOR BROWN, LOW-V GLAZING, NO GRILL OR EQUAL
- 0211 CONCRETE/PAINTED FINISH/CLADDING SYSTEM, COLOR BROWN, LOW-V GLAZING, NO GRILL OR EQUAL
- 0212 CONCRETE/PAINTED FINISH/CLADDING SYSTEM, COLOR BROWN, LOW-V GLAZING, NO GRILL OR EQUAL
- 0213 CONCRETE/PAINTED FINISH/CLADDING SYSTEM, COLOR BROWN, LOW-V GLAZING, NO GRILL OR EQUAL
- 0214 METAL COLLARS PAINTED TO COLOR MATCH EXISTING EXTERIOR DETAIL, 1" BROWN OR EQUAL
- 0215 WOOD JOIST PAINTED BURNING OAK/PAINT CLADDING SYSTEM, COLOR BROWN, LOW-V GLAZING, NO GRILL OR EQUAL
- 0216 METAL COLLARS PAINTED TO COLOR MATCH EXISTING EXTERIOR DETAIL, 1" BROWN OR EQUAL

C D A
 Creative Design Associates, Inc.
 17200 Via de Encanto Street
 City of Industry - CA 91748
 PH: 626-333-8881
 FAX: 626-333-8882

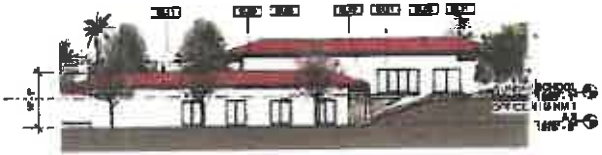
Architectural & Civil Design
 Planning
 Designed By Creative Design Associates, Inc.

Project:
**I KUAN TAO
 TEMPLE**
 COUNTY OF SANBORN COUNTY, CALIFORNIA
 SHEET NO. 021-1
 07/2016
READY FOR CONSTRUCTION
 1/2" = 1'-0" (SCALE)
 1/8" = 1'-0" (SCALE)
 1/16" = 1'-0" (SCALE)

NOT FOR
 CONSTRUCTION
 OR
 RECORDING

Project Title:
BLDG. ELEVATIONS
 CDA Project No. 200
 Date: 08/2016
 Project: 02
 Checked By: CDA
 Drawn By: CDA
 Project & Reference
 Information

Sheet No.:
A-205



4 BLDG 5 SCHOOL & ADMINISTRATION WEST ELEVATION
 SCALE: 1/8" = 1'-0"



3 BLDG 5 SCHOOL & ADMINISTRATION SOUTH ELEVATION
 SCALE: 1/8" = 1'-0"



2 BLDG 5 SCHOOL & ADMINISTRATION NORTH ELEVATION
 SCALE: 1/8" = 1'-0"



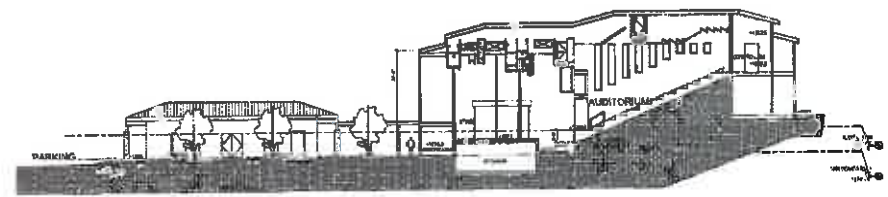
1 BLDG 5 SCHOOL & ADMINISTRATION EAST ELEVATION
 SCALE: 1/8" = 1'-0"

C D A

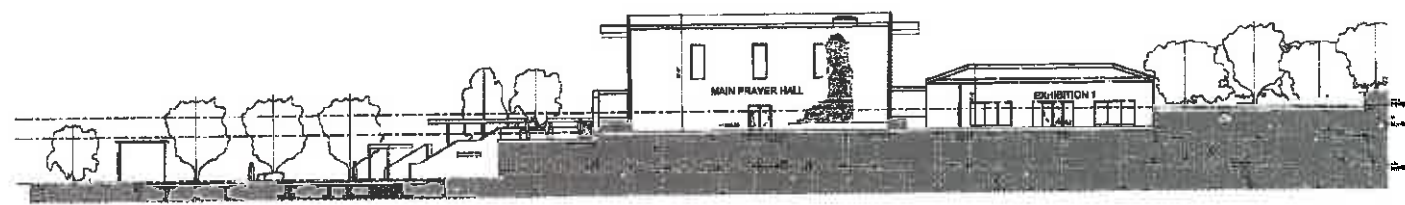
Creative Design Associates, Inc.
17920 Blue Hill Road Street
Croydon, Virginia, VA 22112
909.426.0700
909.426.0701

Architectural - 1/2014 Design
2/15/14

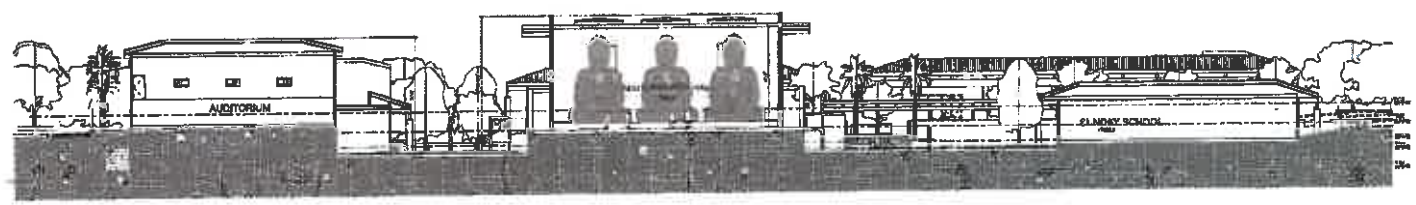
Copyright © Creative Design Associates, Inc.



SECTION
SECTION 3



SECTION
SECTION 2



SECTION
SECTION 1

PROJECT
**I KUAN TAO
TEMPLE**

OWNER: CHURCH OF CHRIST AT A CORNER,
FARMERS DA

DATE:
BRUSH CONSTRUCTION
1500 4000 RE SITE ON
1000 0000 00 00 00
1/15/2014



**NOT FOR
CONSTRUCTION
OR
RECORDING**

Drawing Title:

SECTIONS

CDM Project No. 1001
Date: 02/15/14
Project: IG
Checked By: CDA
Drawn By: CDA
Project: IG
Reference:
Rev: 0000

PAGE BREAK





AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

April 1, 2021

Rob Gonzalez, Project Planner
Riverside County Planning Division
4080 Lemon Street, 12th Floor
Riverside CA 92501

CHAIR
Russell Betts
Desert Hot Springs

VICE CHAIR
Steven Stewart
Palm Springs

COMMISSIONERS

Arthur Butler
Riverside

John Lyon
Riverside

Steve Manos
Lake Elsinore

Richard Stewart
Moreno Valley

Gary Youmans
Temecula

STAFF

Director
Paul Rull

Simon A. Housman
Daniel Zerda
Barbara Santos

County Administrative Center
4080 Lemon St., 14th Floor.
Riverside, CA 92501
(951) 955-5132

www.rcaluc.org

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –
DIRECTOR’S DETERMINATION**

File No.: ZAP1460MA21
Related File No.: TPM37869 (Tentative Parcel Map)
APN: 457-250-051
Airport Zone: Compatibility Zone E

Dear Mr. Gonzalez:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed County of Riverside Case No. TPM37869 (Tentative Parcel Map No. 37869), a proposal to divide 4.82 gross acres located on the southwest corner of Leon Road and Bench Road into two residential lots.

The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, residential density is not restricted.

Although the project is located within the March Air Reserve Base/Inland Port Airport Influence Area, the nearest runway is actually Runway 5-23 at Hemet Ryan Airport. The elevation of Runway 5-23 at Hemet Ryan Airport is approximately 1,499 feet above mean sea level (AMSL) at its southwesterly terminus. At a distance of 28,640 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with an elevation at top of roof exceeding 1,785 feet AMSL. The project site elevation is 1,714 feet AMSL. No building permits for new structures are in process at this time, and review by the Federal Aviation Administration Obstruction Evaluation Services (FAA OES) is not a prerequisite to land division. Therefore, FAA OES review for height/elevation reasons was not required.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, provided that the County of Riverside applies the following recommended conditions:

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be

AIRPORT LAND USE COMMISSION

downward facing.

2. The following uses/activities are not included in the proposed project and shall be prohibited at this site.
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Hazards to flight.
3. The attached notice shall be provided to all potential purchasers, lessees, and/or tenants of the property.
4. Any proposed detention basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the detention basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the

AIRPORT LAND USE COMMISSION

name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

If you have any questions, please contact me at (951) 955-6893

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Paul Rull, ALUC Director

Attachments: Notice of Airport in Vicinity

cc: Procopio and Hermelinda Jimenez (applicant/property owner)
Daniel Jimenez (representative)
Gary Gosliga, Airport Manager, March Inland Port Airport Authority
Doug Waters, Deputy Base Civil Engineer, March Air Reserve Base
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1460MA21\ZAP1460MA21.LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

**THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS**

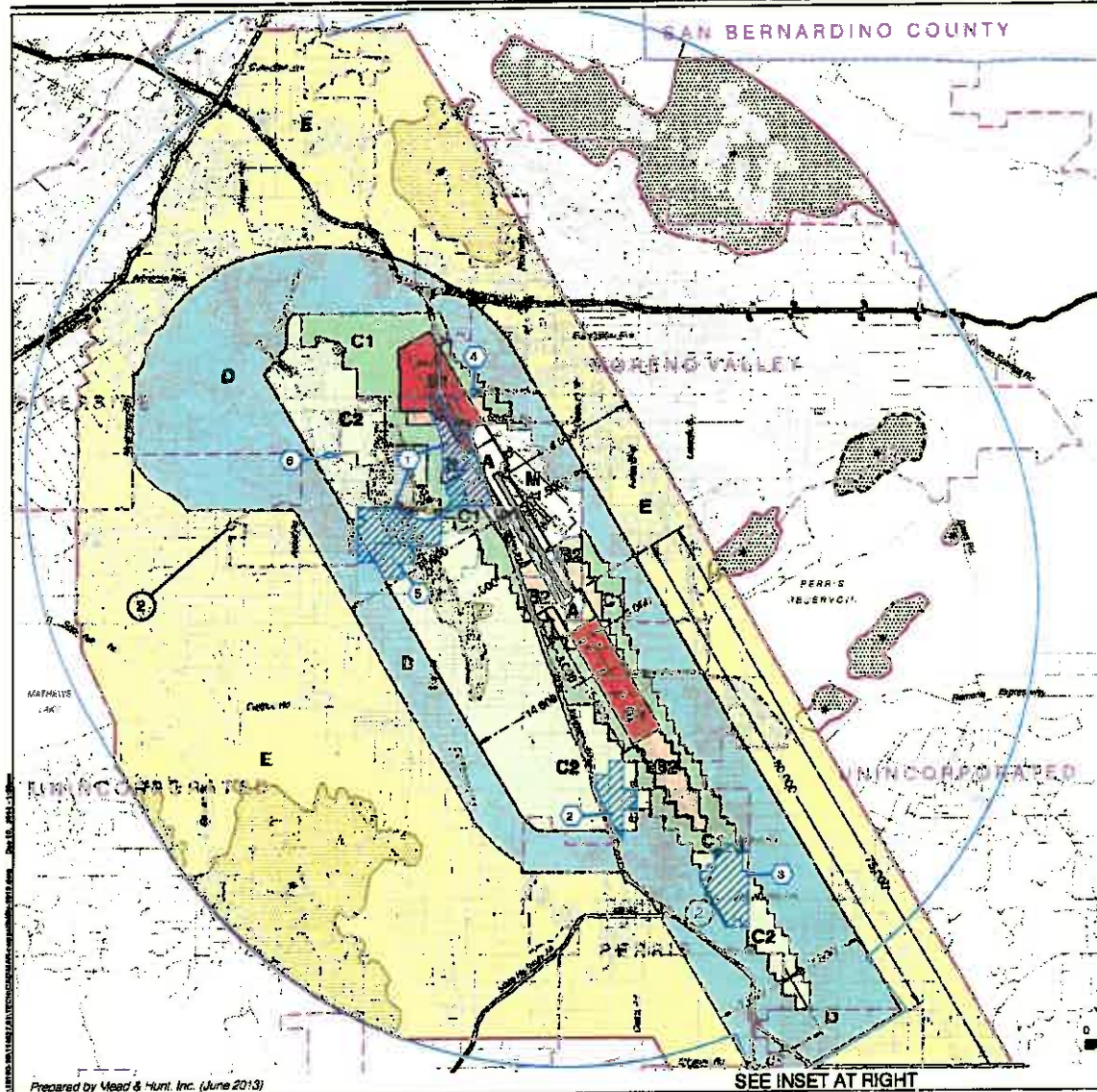
**PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES**



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____

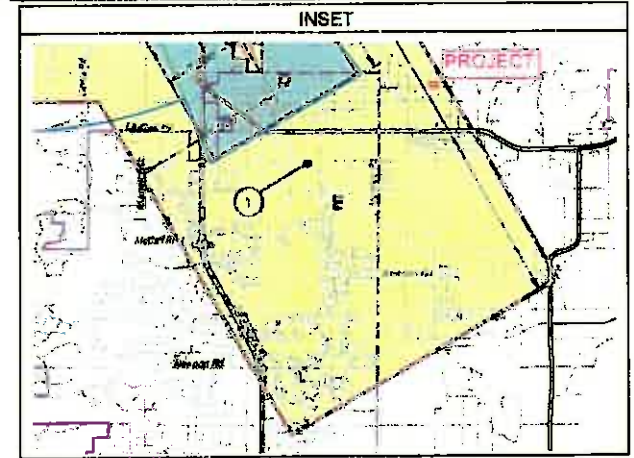


LEGEND

- Compatibility Zones**
- Airport Influence Area Boundary
 - Zone A
 - Zone B1
 - Zone B2
 - Zone C1
 - Zone C2
 - Zone D
 - Zone E
 - Zone M
 - High Terrain Zone
 - FAR Part 77 Military Outer Horizontal Surface Limits
 - FAR Part 77 Notification Area

- Boundary Lines**
- March Air Reserve Base / Air Force Property
 - March Joint Powers Authority Property Line
 - County Boundary
 - City Limits
 - Site-Specific Exceptions (existing local agency commitments to development projects)
 - 1 March JPA: March Business Center/Veridian
 - 2 Perris: Harvest Landing
 - 3 Perris: Park West
 - 4 Moreno Valley: Affordable Housing
 - 5 March JPA: Ben Clark Training Center
 - 6 Riverside: Ridge Crest Subdivision

- 1 Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end. Airport Elevation is 1,535 feet MSL
- 2 Point at which departing aircraft typically reach 3,000 feet above runway end



Note:
All dimensions are measured from runway ends and centerlines



Base map source: County of Riverside 2013

Riverside County
Airport Land Use Commission
March Air Reserve Base / Inland Port Airport
Land Use Compatibility Plan
(Adopted November 13, 2014)

Map MA-1

Compatibility Map
March Air Reserve Base / Inland Port Airport

Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map My County Map

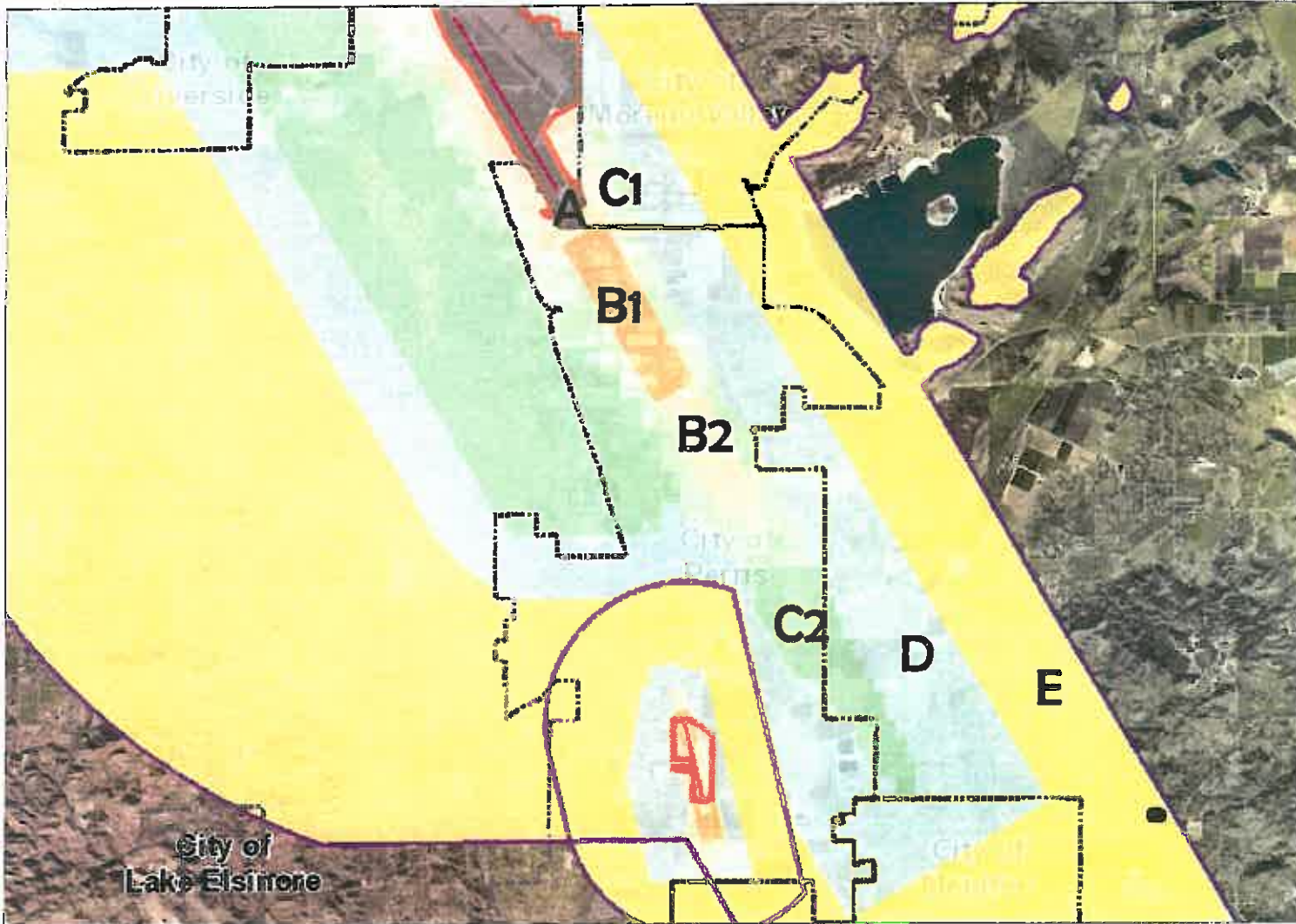
Los Angeles



San Diego

Tijuana

Mexico



Legend

- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5
- C2-EXC6



IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.



REPORT PRINTED ON... 3/29/2021 10:21:33 AM

© Riverside County GIS

Notes

Map My County Map



- Legend**
- Parcels
 - Runways
 - Airports
 - Airport Influence Areas
 - Airport Compatibility Zones**
 - OTHER COMPATIBILITY ZONE
 - A
 - A-EXC1
 - B1
 - B1-APZ I
 - B1-APZ I-EXC1
 - B1-APZ II
 - B1-APZ II-EXC1
 - B1-EXC1
 - B2
 - B2-EXC1
 - C
 - C1
 - C1-EXC1
 - C1-EXC3
 - C1-EXC4
 - C1-HIGHT
 - C2
 - C2-EXC1
 - C2-EXC2
 - C2-EXC3
 - C2-EXC5



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0 376 752 Feet

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Notes

Map My County Map

Los Angeles



San Diego

Tijuana

Mexicali

Legend

-  Parcels
-  County Centerline Names
-  County Centerlines
-  Blueline Streams
-  City Areas
-  World Street Map



Notes



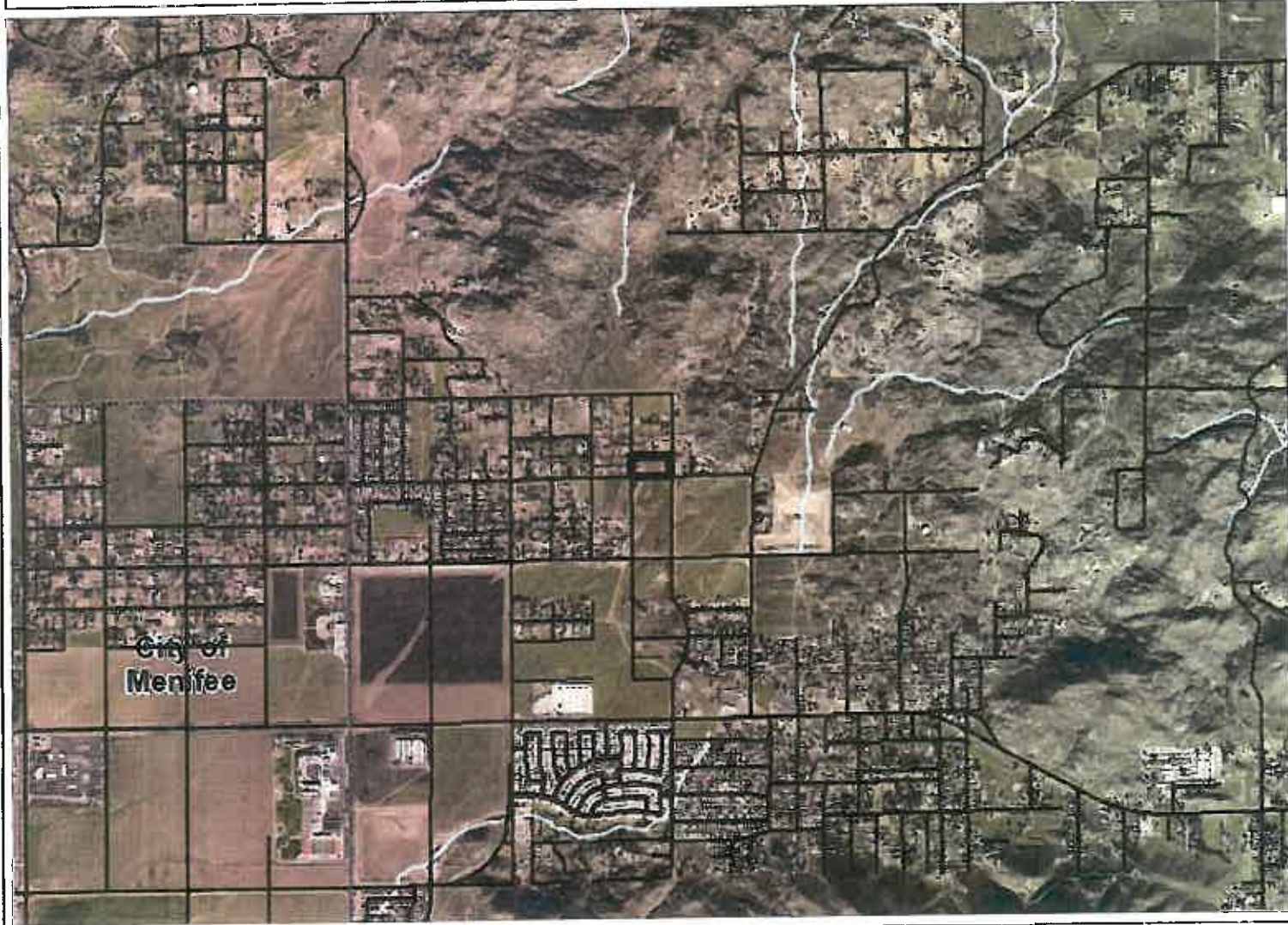
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

0 376 752 Feet

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Map My County Map



Legend

- County Centerlines
- Blue line Streams
- City Areas
- World Street Map

Notes



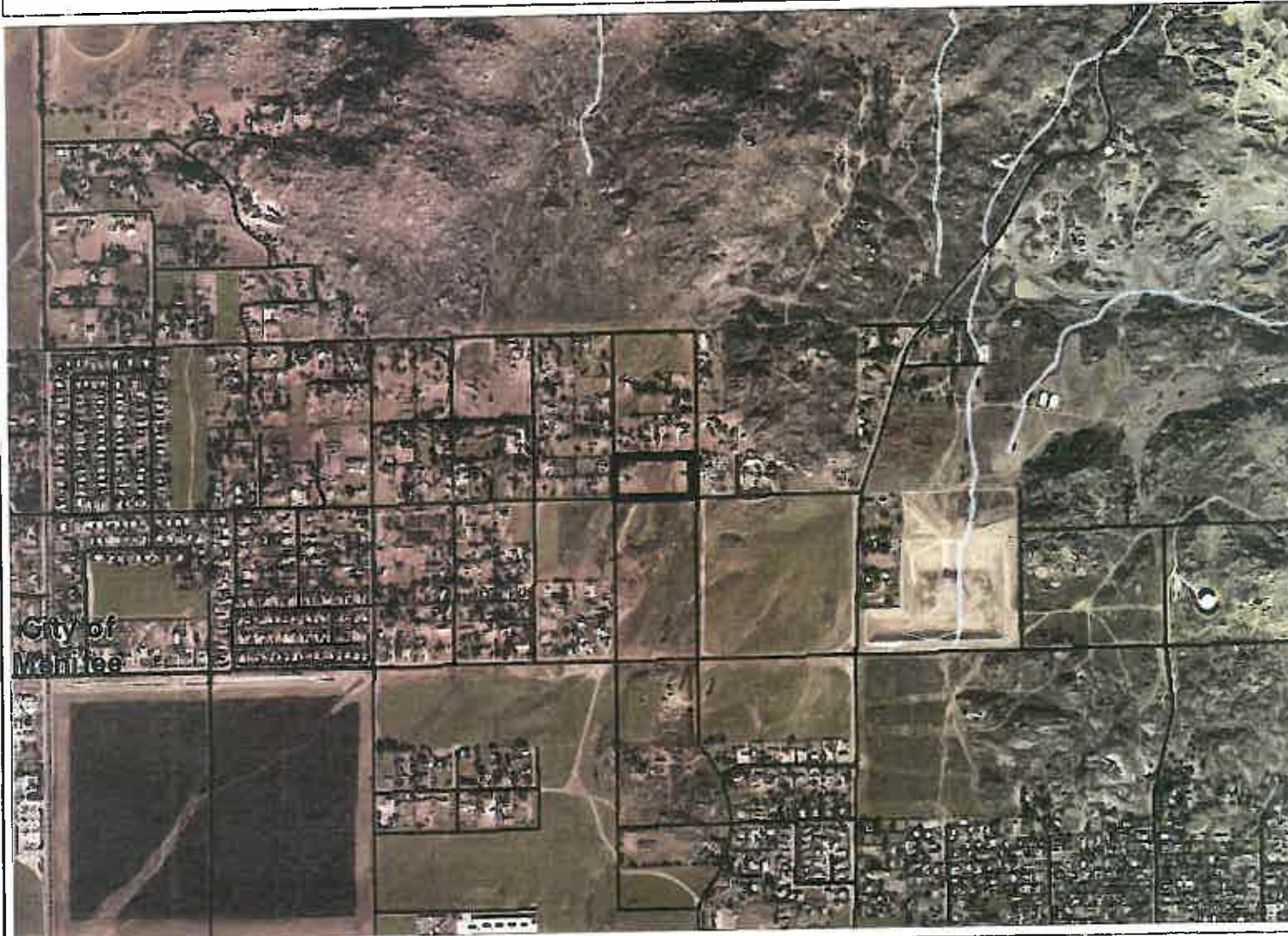
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.



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Map My County Map



- Legend**
- County Centerlines
 - Blue Line Streams
 - City Areas
 - World Street Map



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Notes



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Map My County Map

Los Angeles



San Diego

Tijuana - Mexico



Legend

-  Parcels
-  County Centerlines
-  Blueline Streams
-  City Areas
-  World Street Map



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0 376 752 Feet

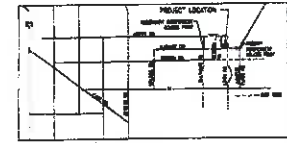
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Notes

IN THE UNINCORPORATED TERRITORY OF THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA
TENTATIVE PARCEL MAP NO. 37869

SCHEDULE - H
 APN: 457-250-051
 25246 JAMAWAG DR.
 HOMELAND, CA 92548



LEGAL DESCRIPTION
 PARCEL 4 OF PARCEL NO. 12712 IN THE COUNTY OF RIVERSIDE, CALIFORNIA
 ACCORDING TO MAP ON FILE IN BOTH SA PAGE 2 OF PARCEL MAPS
 RECORDS OF RIVERSIDE COUNTY

T.O.G. FOR SECTION 7

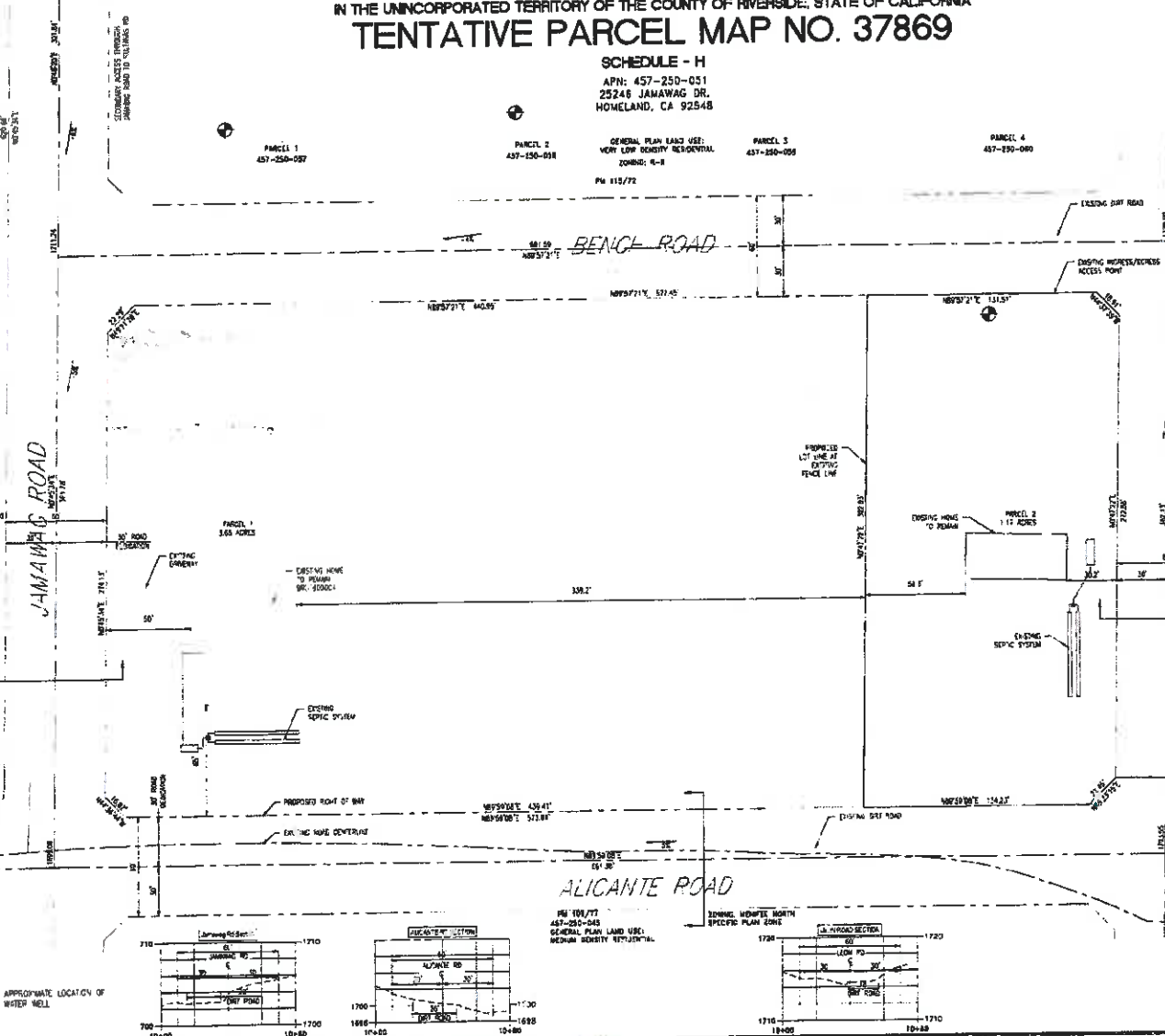
1. AN EASEMENT FOR PAVEMENT, MESH DRIVE AND RIGHTS RESERVED
 HEREIN AS PROVIDED IN AN INSTRUMENT
 RECORDED 1/10/1984 AS INSTRUMENT NO. 1983-02241 OF
 THE PUBLIC UTILITIES, HOUSES, EDGES AND UTILITIES DIVISIONS
 IN FAVOR OF CRYSTAL TELEPHONE COMPANY OF CALIFORNIA
 INTERESTS, THE SUCCESSORS TO THE SA LAND.

2. AN INSTRUMENT UPON THE TERMS AND CONDITIONS CONTAINED THEREIN
 ENTITLED NOTICE OF ENVIRONMENTAL RESTRICTIONS, PARCEL 2
 RECORDED 10/24/2016 AS INSTRUMENT NO. 2016-482318 OFFICIAL
 RECORDS 457-250-038

3. AN INSTRUMENT UPON THE TERMS AND CONDITIONS CONTAINED THEREIN
 ENTITLED NOTICE OF ENVIRONMENTAL RESTRICTIONS
 RECORDED 11/2/2016 AS INSTRUMENT NO. 2016-482664 OFFICIAL
 RECORDS 457-250-038

4. NOTICE OF MANUFACTURED HOME (MOBILE HOMES) OF DOMESTIC COMPLY
 INSTALLATION ON A TIE-RODDED SYSTEM IN ACCORDANCE WITH CALIFORNIA
 HEALTH AND SAFETY CODE TITLE 24 (HCS24350)

APPROXIMATE LOCATION OF
 WATER WELL



DEEDS AND NET LOT SIZE
 PARCEL 1
 GROSS 3.85 AC.
 NET 3.12 AC.
 PARCEL 2
 GROSS 1.17 AC.
 NET 1.03 AC.

ENGINEERS
 BLUE ENGINEERING AND CONSULTING, INC.
 ANSEL CEAR, P.E.
 12233 MIDLAND AVE #100-504
 RANCHO CUCAMONCA, CA 91739
 909-248-8827
 ANSEL@BLUEENGIN.COM

OWNER/REPRESENTATIVE
 ANSEL JIMENEZ
 25246 JAMAWAG DR.
 HOMELAND, CA 92548
 714-391-6807

UTILITY PROVIDERS
WATER
 EASTERN MUNICIPAL WATER DISTRICT
 2270 TRUMBULE ROAD
 PERRIS, CA 92570
 951-326-3777
 CLOSEST WATER MAIN TO SITE IS AT THE
 INTERSECTION OF PERRIS RD AND
 ALICANTE DR. APPROXIMATELY 500 FEET
 AWAY.

SEWER
 EASTERN MUNICIPAL WATER DISTRICT
 2270 TRUMBULE ROAD
 PERRIS, CA 92570
 951-326-3777
 CLOSEST SEWER MAIN TO THE SITE IS AT
 THE INTERSECTION OF ALICANTE DRIVE
 AND SALVADORS ROAD APPROXIMATELY
 1,500 FEET AWAY.

NATURAL GAS
 SOUTHERN CALIFORNIA GAS COMPANY
 P.O. BOX 3000
 REDLANDS, CA

ELECTRICITY
 SOUTHERN CALIFORNIA Edison
 28100 MENIFEE POINTE
 RONDOLAND, CA 92530

TELEPHONE
 VERIZON
 1515 JAMAWAG STREET
 HOME, CA 92543

CABLE TELEVISION
 THE BARNER CABLE
 4077 WEST SUTTON AVE
 HOME, CA 92543

NO COUNTY SERVICE AREA OR
 COMMUNITY FACILITIES (EXCEPT FOR THIS
 PROJECT)

NO RECREATION OR PARK DISTRICT OF
 COUNTY SERVICE AREA (EXCEPT FOR THIS
 PROJECT)

NO.	REVISIONS	DATE	BY

PARCEL 1
 457-250-050

SCALE 1" = 50'

GENERAL PLAN LAND USE:
 VERY LOW DENSITY RESIDENTIAL
 ZONING: R-8

GENERAL NOTES:

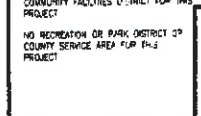
- APN 457-250-051
- EXISTING LAND USE DESIGNATION VERY LOW DENSITY RESIDENTIAL
- EXISTING ZONING R-8
- THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) FLOOD INSURANCE RATE MAP (FIRM) FOR RIVERSIDE COUNTY CALIFORNIA COMMUNITY NAME NUMBER 50002-14600 EFFECTIVE DATE OF AUGUST 31, 2005 INDICATES A "ZONE X" BASED UPON AN APPROXIMATE SITE LOCATION
- THINGS OF BEAUTY (TOB) (STATE CODE) ZONE 634 12 & 13
- THE NEARBY PARCELS WHO CONTAIN EXISTING CONCRETE DRIVEWAYS OF LAND DIVISION
- EXISTING WATER WELL STRUCTURE LOCATED AT THE NEAREST CORNER OF THE SITE TO "DOWN"
- TOPOGRAHY SURVEY SOURCE PROFESSIONAL LAND SURVEYING NO. 110 IN SANJUAN ARE SITE #1-5 PERM, CA 92571
- SEPTIC SYSTEM IS EXISTENT SERVING THE SITE AND WILL CONTINUE SERVING THE EXISTING HOMES
- THE PROPERTY IS WITHIN UNINCORPORATED/HOMELAND 140 COUNTY SERVICE AREA
- NO COMMON AREAS, MOBILE HOMES, OPEN SPACE OR RECREATIONAL AREAS ARE BEING PROPOSED FOR THIS PROJECT
- TWO DWELLING UNITS ARE EXISTING ON SITE. NO UNITS ARE BEING PROPOSED ON THIS PROJECT
- BASED ON A PRELIMINARY REVIEW, LAND IS NOT SUBJECT TO EROSION OR SLIDE OR OTHER GEOLOGIC HAZARD. LAND IS NOT WITHIN A SPECIAL STUDY ZONE OF ACT LAND IS SUBJECT TO OVERFLOW, SEPARATION OF FLOOD HAZARDS & GEOTECHNICAL REPORT WILL BE REQUIRED TO VERIFY FINDINGS.

PARCEL 2
 457-250-049

GENERAL PLAN LAND USE:
 MEDIUM DENSITY RESIDENTIAL

DATE SUBMITTED MAP PREPARED
 08/27/2025

COUNTY OF RIVERSIDE
 JENNIFER N.



COUNTY OF RIVERSIDE
 BUILDS & SAFETY DEPARTMENT

PROJECT NO. _____ DATE _____

APPROVED: _____

PAGE BREAK





AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

April 14, 2020

Mr. Sean Kelleher, Project Planner
City of Moreno Valley Planning Department
14177 Frederick Street
Moreno Valley CA 92552

CHAIR
Steven Stewart
Palm Springs

VICE CHAIR
Steve Marcos
Lake Elsinore

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –
DIRECTOR’S DETERMINATION**

COMMISSIONERS

Arthur Butler
Riverside

John Lyon
Riverside

Russell Betts
Desert Hot Springs

Richard Stewart
Moreno Valley

Gary Youmans
Temecula

STAFF

Director
Paul Rull

Simon A. Housman
Daniel Zorda
Barbara Santos

County Administrative Center
4000 Leron St., 14th Floor
Riverside, CA 92501
(951) 925-6132

File No.: ZAP1463MA21
Related File No.: PEN20-0095 (General Plan Amendment), PEN20-0096 (Change of Zone), PEN21-0066 (Conditional Use Permit)
APN: 256-150-001
Airport Zone: Compatibility Zone E

Dear Mr. Kelleher:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to ALUC Resolution No.15-01 (as adopted on August 13, 2015), and pursuant to Policy 1.5.2(d) staff reviewed City of Moreno Valley Case Nos. PEN20-0095 (General Plan Amendment), PEN20-0096 (Change of Zone), PEN21-0066 (Conditional Use Permit), a proposal to develop 108 detached condominium units on 17.3 acres within a 32.7 acre parcel located easterly of Gemert Road, northerly of Jennings Court, westerly of Hilmer Court, and southerly of the Box Springs Mountain Reserve Park. The applicant also proposes to amend 17.3 acres of the site’s General Plan land use designation from Residential: Max 2 DU/AC to Residential: Max 10 DU/AC, and to amend the remaining 15.4 acres of the site’s General Plan land use designation from Hillside Residential to Open Space, as well as a change the zoning of 17.3 acres from R2-2DU/AC to RS10-10DU/AC, and a change to the zoning of the remaining 15.4 acre portion of the site from Hillside Residential to Open Space.

The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, residential density and non-residential intensity are not restricted.

The elevation of Runway 14-32 at March Air Reserve Base/Inland Port Airport is approximately 1,535 feet above mean sea level (AMSL) at its northerly terminus. At a distance of 23,300 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with an elevation at top of roof exceeding 1,768 feet AMSL. The project site elevation is 1,680 feet AMSL, and the tallest proposed structure is 35 feet, for a maximum top point elevation of 1,715 feet AMSL. There are no proposed changes to the existing building height. Therefore, Federal Aviation Administration (FAA) obstruction evaluation review for height/elevation reasons is not required.

www.aluc.org

AIRPORT LAND USE COMMISSION

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, provided that the City of Moreno Valley applies the following recommended conditions:

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Other Hazards to flight.
4. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property.
5. Any proposed detention basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the detention basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available

AIRPORT LAND USE COMMISSION

at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

If you have any questions, please contact me at (951) 955-6893.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Paul Rull, ALUC Director

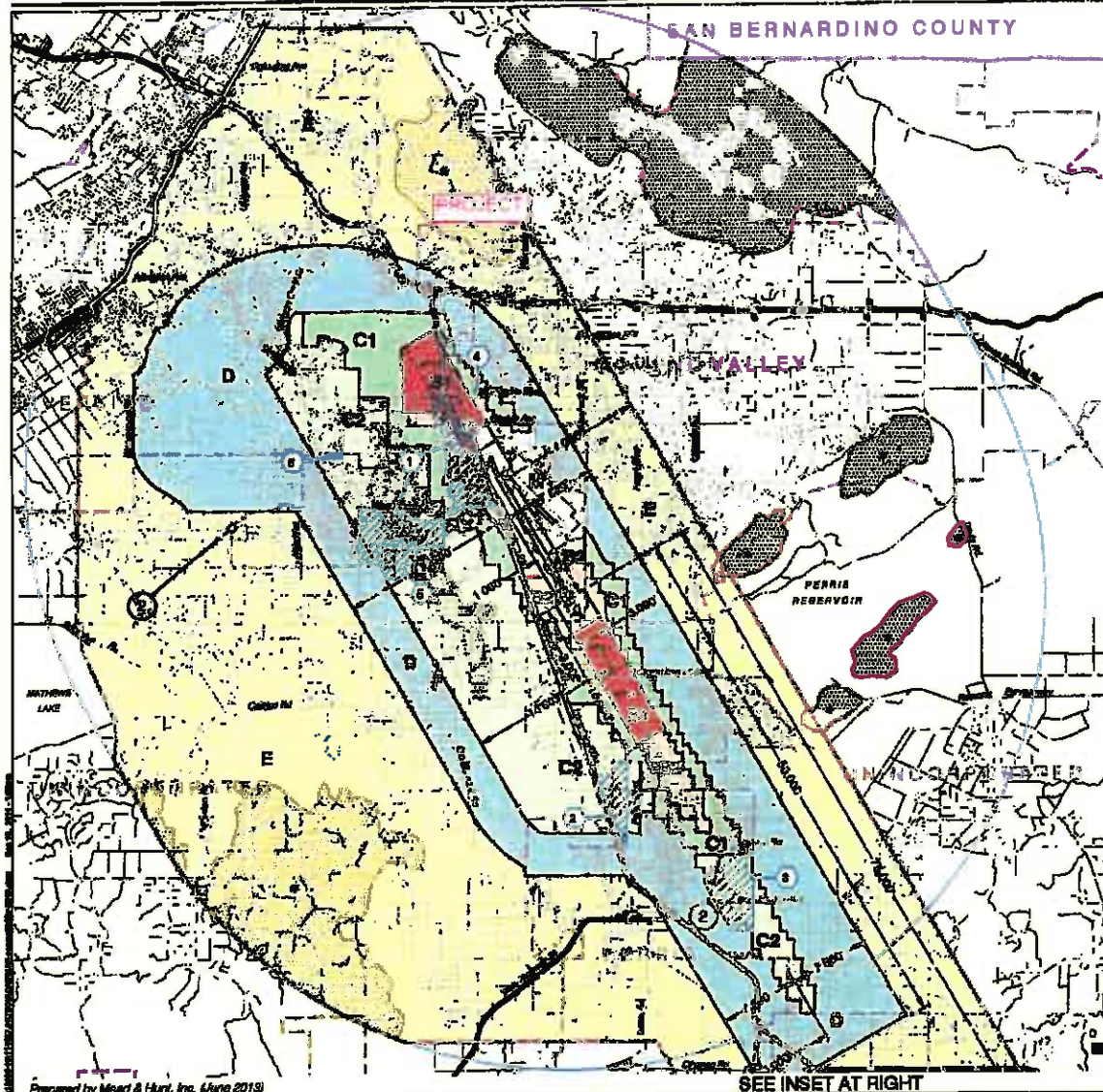
Attachments: Notice of Airport in Vicinity

cc: United Engineering Group (applicant)
Jason Ackerman (representative)
Shizao Zheng (property owner)
Gary Gosliga, Airport Manager, March Inland Port Airport Authority
Doug Waters, Deputy Base Civil Engineer, March Air Reserve Base
ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1463MA21\ZAP1463MA21.LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)



LEGEND

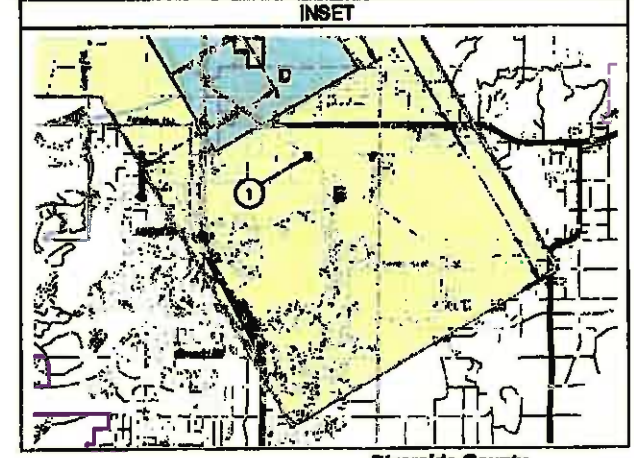
Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C1
- Zone C2
- Zone D
- Zone E
- Zone M
- High Terrain Zone
- FAR Part 77 Military Outer Horizontal Surface Limits
- FAR Part 77 Notification Area

- 1 Point at which aircraft on Runway 30 (LS) approach descend below 3,000 feet above runway end. Airport Elevation is 1,833 feet MSL.
- 2 Point at which departing aircraft typically reach 3,000 feet above runway end.

Boundary Lines

- March Air Reserve Base / Air Force Property
- March Joint Powers Authority Property Line
- County Boundary
- City Limits
- Site-Specific Exceptions (existing local agency commitments to development projects)
 - March JPA: March Business Center/Meridian
 - Perris: Harvest Landing
 - Perris: Park West
 - Moreno Valley: Affordable Housing
 - March JPA: Ben Clark Training Center
 - Riverside: Ridge Crest Subdivision



Riverside County
 Airport Land Use Commission
 March Air Reserve Base / Inland Port Airport
 Land Use Compatibility Plan
 (Adopted November 13, 2014)

Note:
 All dimensions are measured from
 runway ends and centerlines.



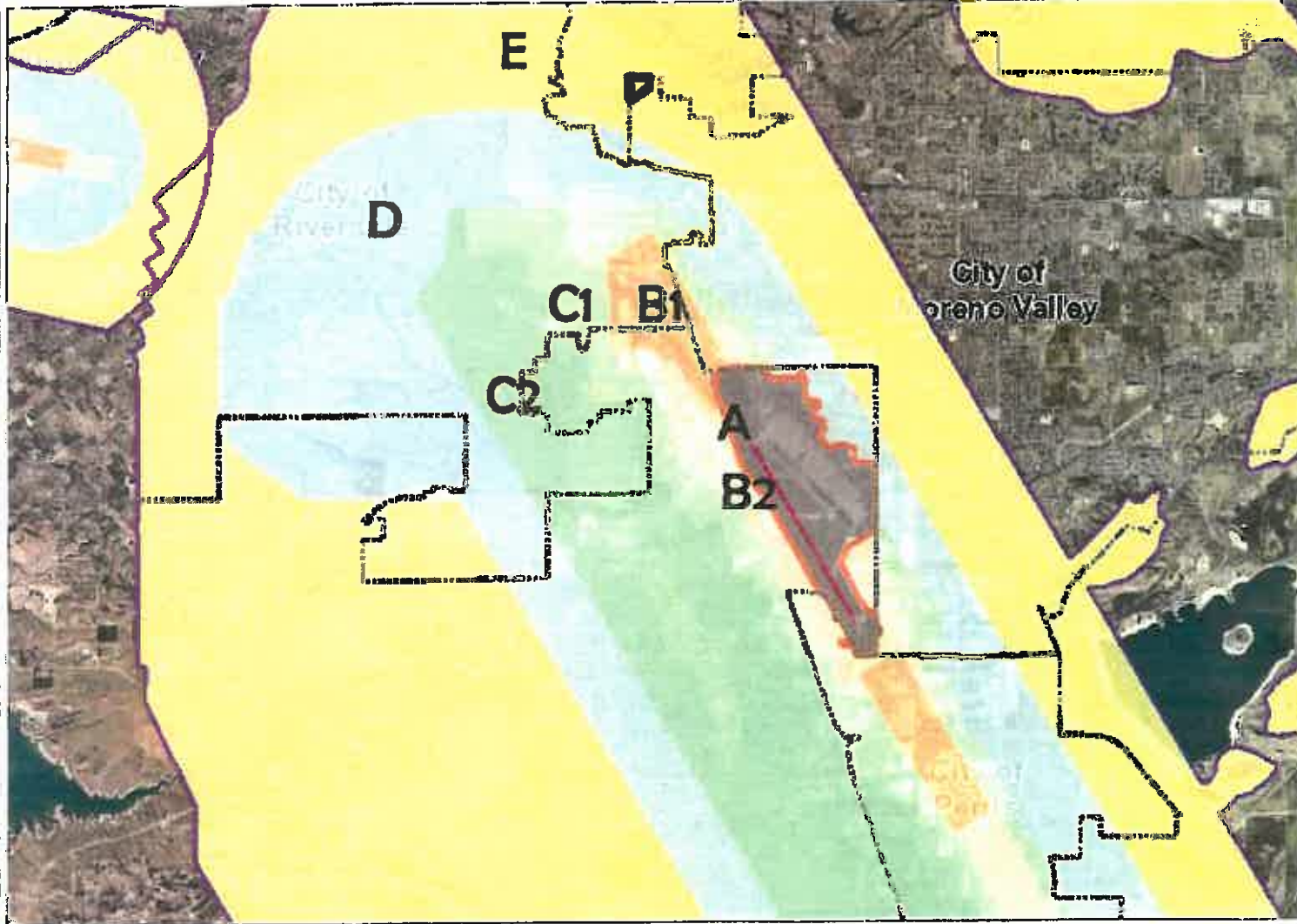
Base map source: County of Riverside 2013

Map MA-1
Compatibility Map
 March Air Reserve Base / Inland Port Airport

Prepared by Mead & Hunt, Inc. (June 2013)

SEE INSET AT RIGHT

Map My County Map



- Legend**
- Runways
 - Airports
 - Airport Influence Areas
 - Airport Compatibility Zones**
 - OTHER COMPATIBILITY ZONE
 - A
 - A-EXC1
 - B1
 - B1-APZ I
 - B1-APZ I-EXC1
 - B1-APZ II
 - B1-APZ II-EXC1
 - B1-EXC1
 - B2
 - B2-EXC1
 - C
 - C1
 - C1-EXC1
 - C1-EXC3
 - C1-EXC4
 - C1-HIGHT
 - C2
 - C2-EXC1
 - C2-EXC2
 - C2-EXC3
 - C2-EXC5
 - C2-EXC6



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Notes



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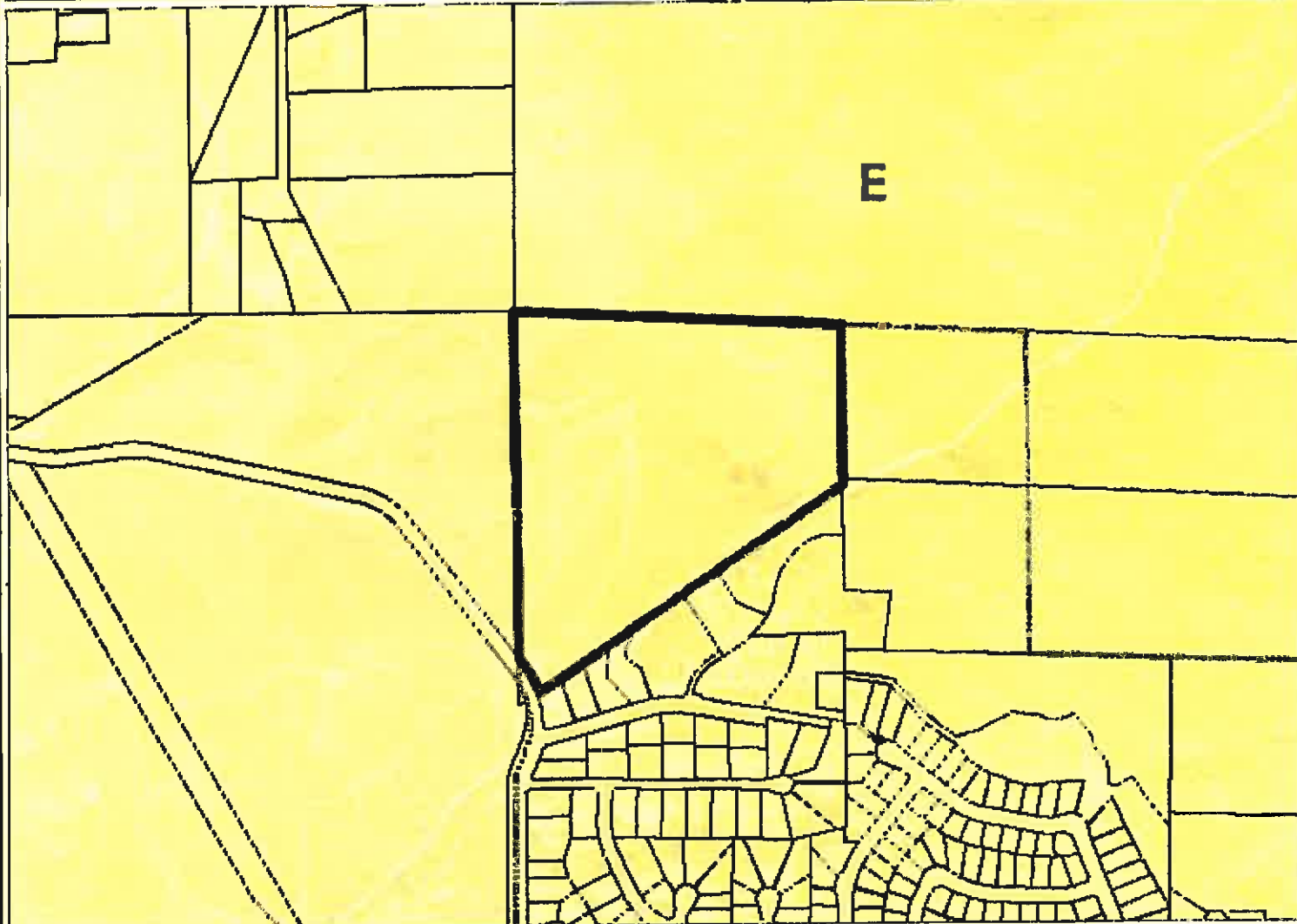
Map My County Map



E

Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE
- A
- A-EXC1
- B1
- B1-APZ I
- B1-APZ I-EXC1
- B1-APZ II
- B1-APZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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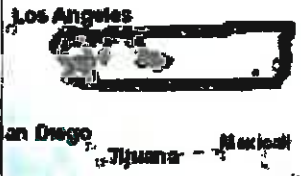


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Notes

Map My County Map



Legend

- Parcels
- County Centerlines
- Blueline Streams
- City Areas
- World Street Map



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Notes



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Map My County Map



Legend

- Blue Line Streams
- City Areas
- World Street Map

Notes



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Map My County Map



Legend

- County Centerlines
- Blue Line Streams
- City Areas
- World Street Map



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0 1 3,009 Feet
505

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Notes

Map My County Map



Legend

-  Parcels
-  County Centerline Names
-  County Centerlines
-  Blueline Streams
-  City Areas
-  World Street Map



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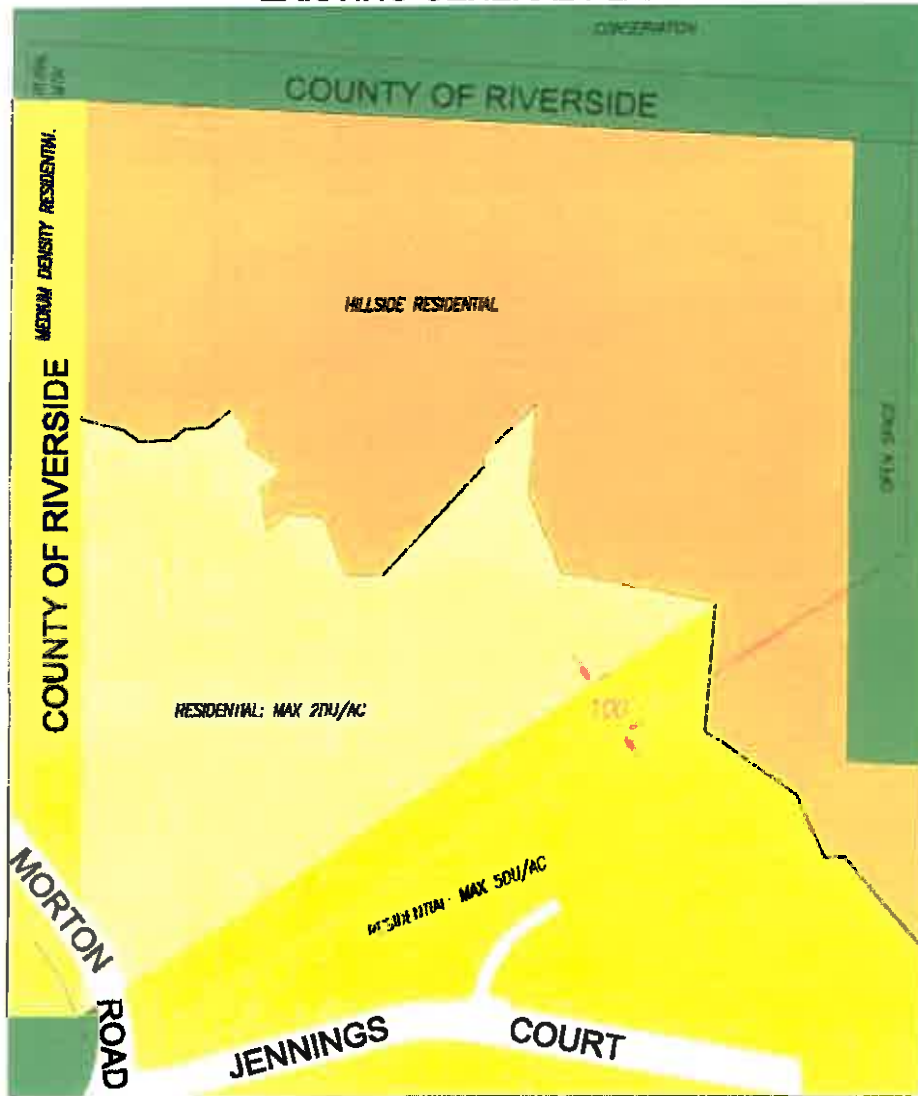
Notes

0 752 1,505 Feet

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EXISTING GENERAL PLAN

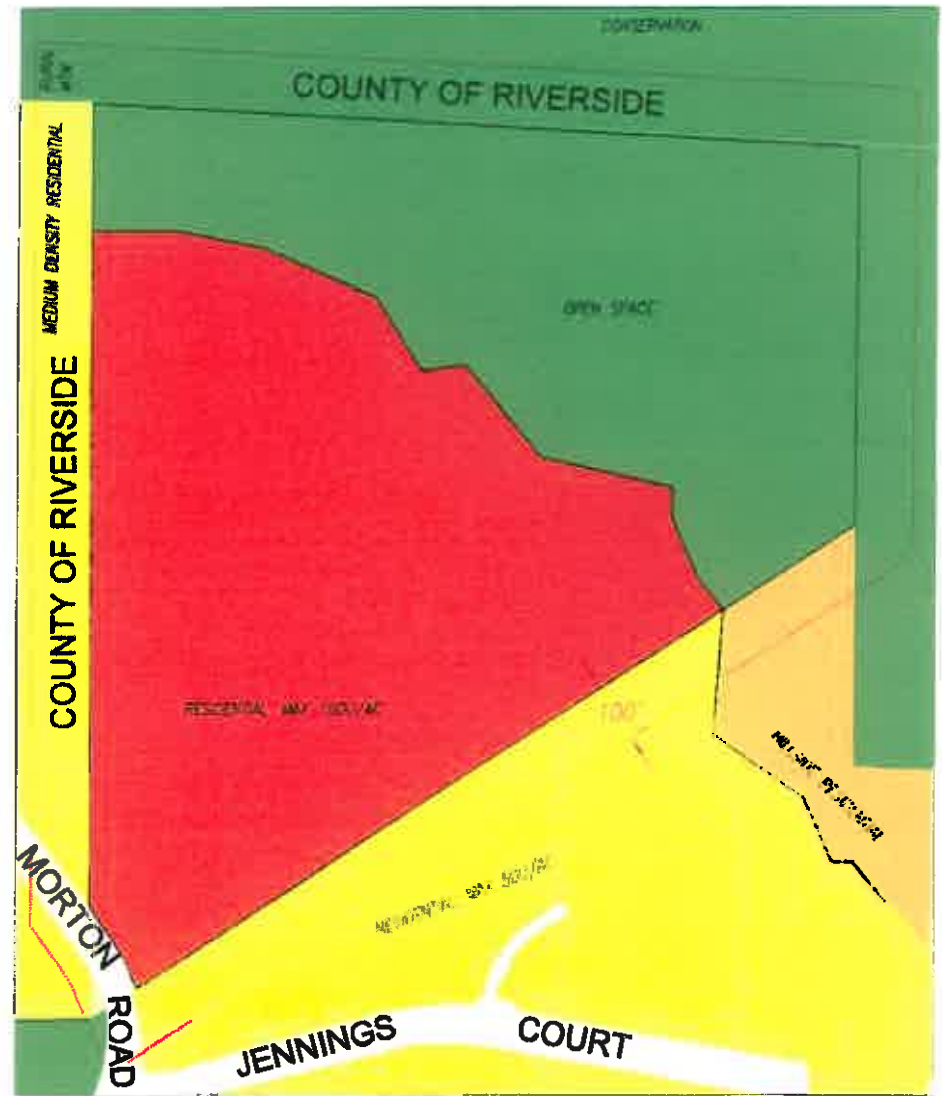


GENERAL PLAN MAP

GATEWAY HEIGHTS

CITY OF MORENO VALLEY, CALIFORNIA

PROPOSED GENERAL PLAN

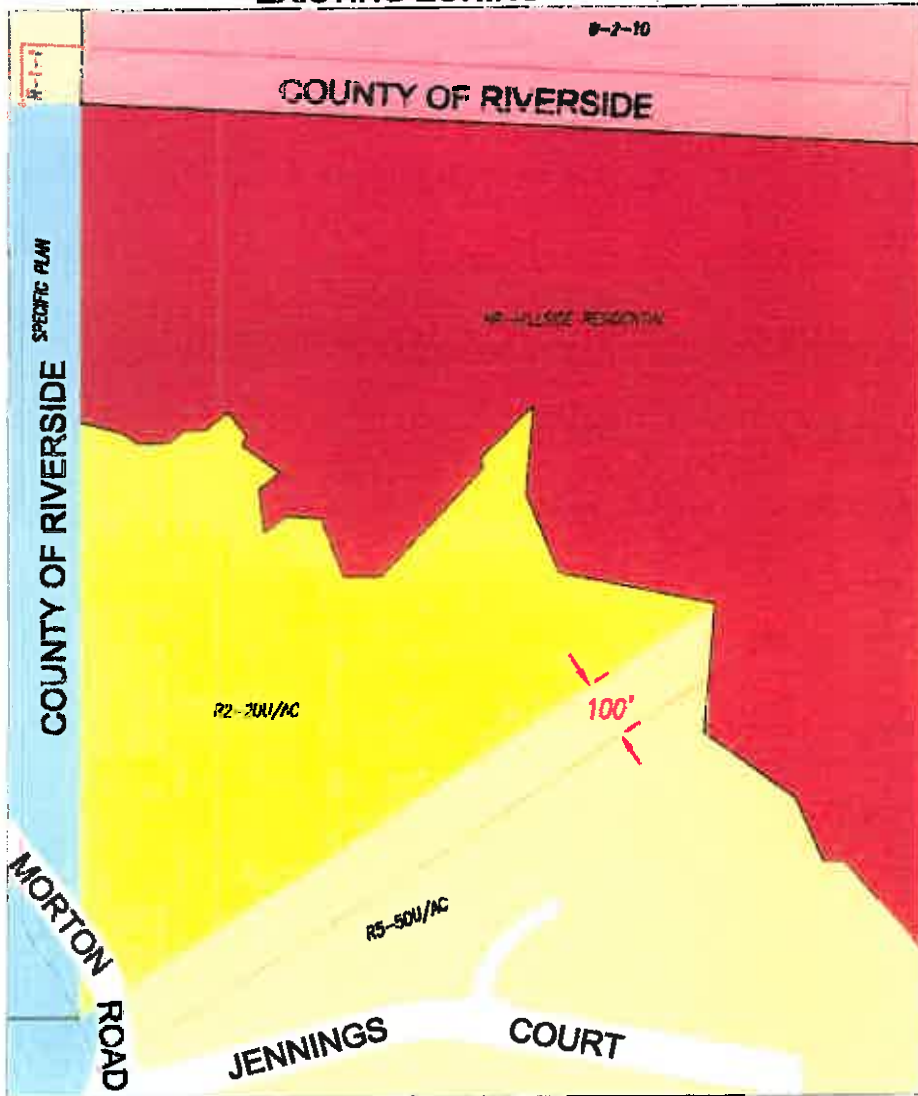


SCALE: 1" = 200'

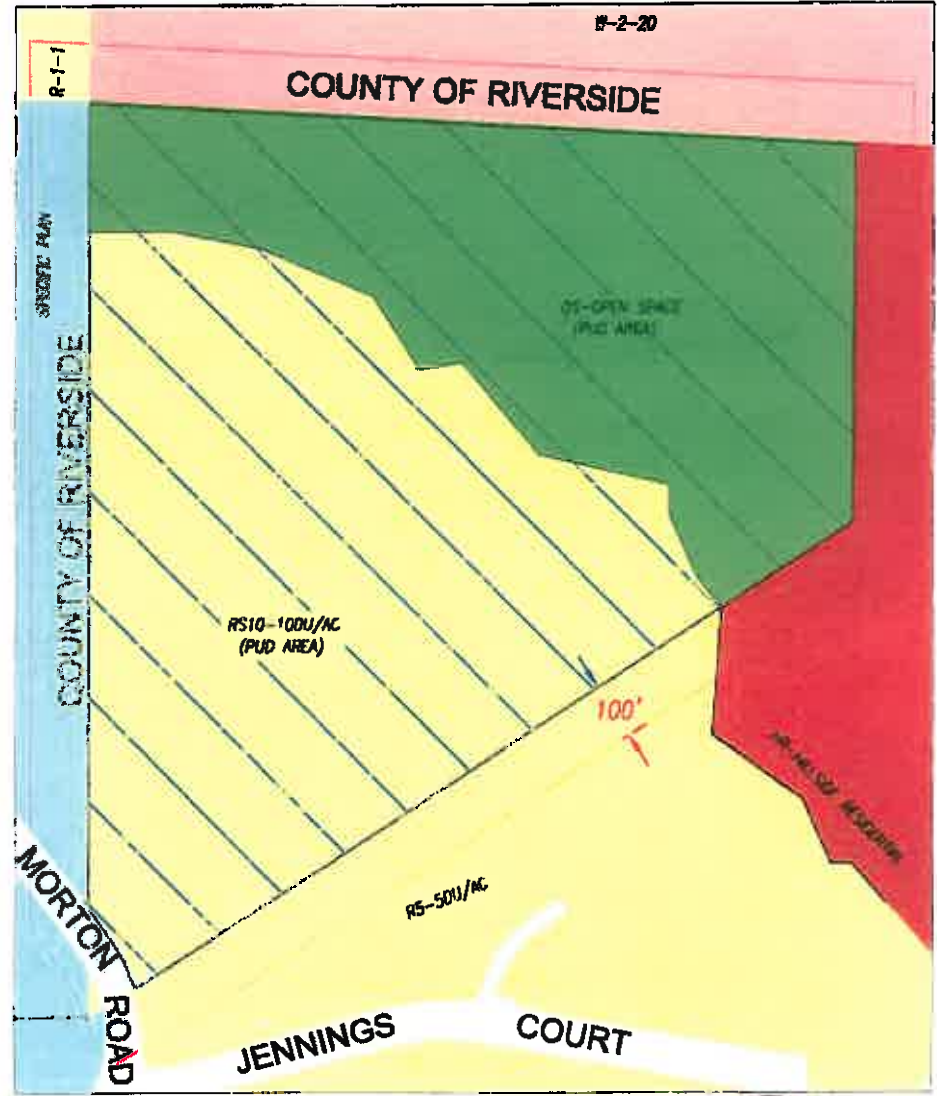


EXHIBIT E

EXISTING ZONING



PROPOSED ZONING



ZONING MAP

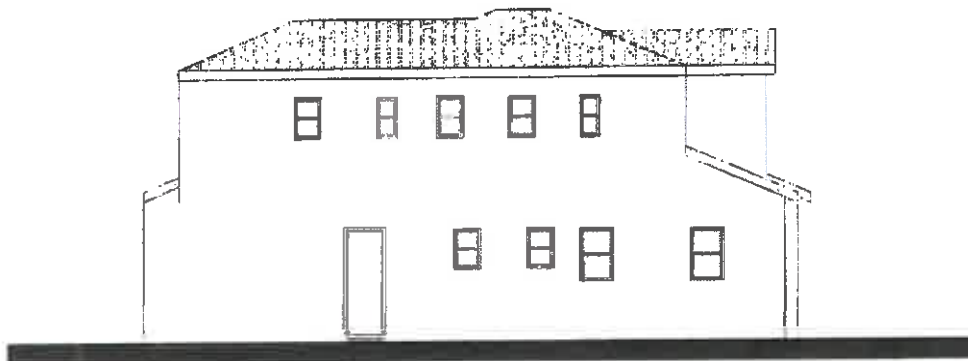
GATEWAY HEIGHTS

CITY OF MORENO VALLEY, CALIFORNIA

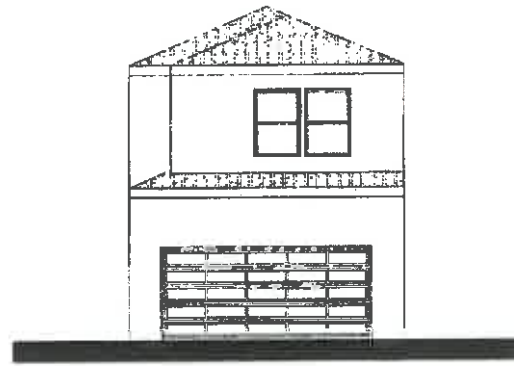
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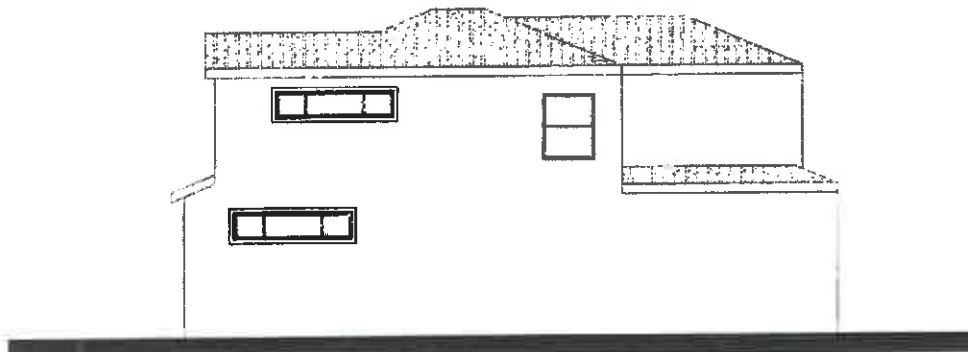
EXHIBIT F



① PLAN 1 LEFT ELEVATION



① PLAN 1 REAR ELEVATION



① PLAN 1 RIGHT ELEVATION



① PLAN 1 FRONT ELEVATION

KPI
 KNITTER PARTNERB
 INTERNATIONAL, INC.
 architecture & planning
 1782 Miramonte, Suite C
 Pasadena, California 92614-8802
 626.782.1177 www.kpi.com

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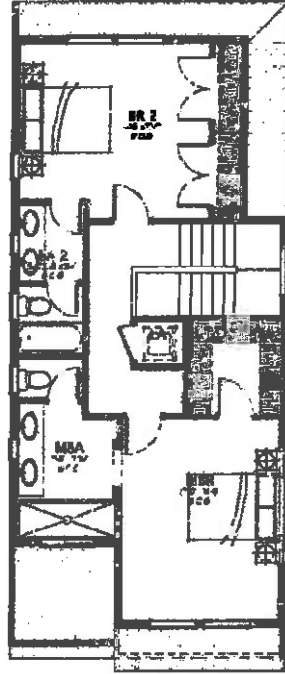
Hengchou Group
 177 E. Colorado BLVD, Ste. 208
 Pasadena, CA 91103
Gateway Heights
 Moreno Valley, USA

PROJECT INFO	
PROJECT NUMBER:	2020
PROJECT MANAGER:	SLC
DRAWN BY:	SH
CHECKED BY:	MS
DATE PLOTTED	

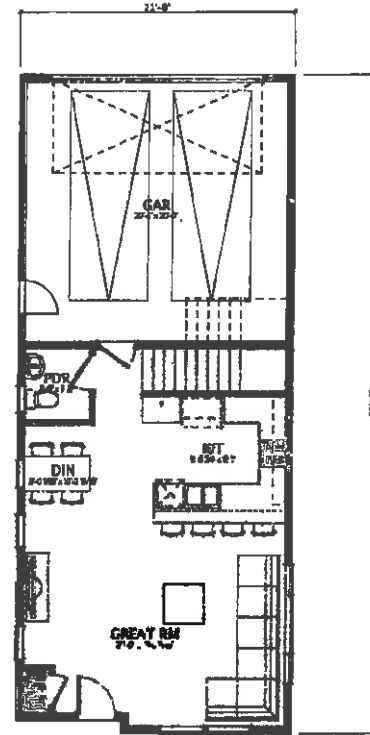
PLAN 1 EXTERIOR

DRAWING TITLE: PLAN 1 EXTERIOR - FRONT ELEVATION - 2020-08-10 - 10:00 AM - 10:00 AM

177 E. Colorado Blvd., Suite 200, Pasadena, CA 91105
 626.782.1177
 www.kpi.com



1 PLAN 1 SECOND FLOOR 785 sq ft



1 PLAN 1 FIRST FLOOR 615 sq ft
TOTAL 1400 sq ft

KPI
 KNITTER PARTNERS
 INTERNATIONAL, INC.
 architecture & planning
 17782 Wiltona North, Suite C
 Irvine, California 92614-8802
 949.782.1177 www.kpi.com
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NO.	DATE	REVISION

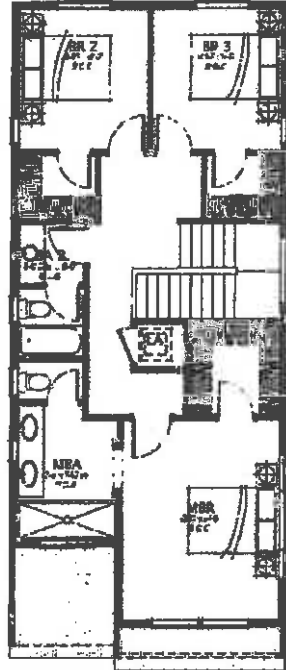
Henghou Group
 177 E. Colorado Blvd., Ste. 200
 Pasadena, CA 91105
Gateway Heights
 Sierra Valley, USA

PROJECT INFO	
PROJECT NUMBER:	209
PROJECT MANAGER:	PL
DESIGNER:	SLW
PROJECT START DATE:	4/02
DATE: 1/04	

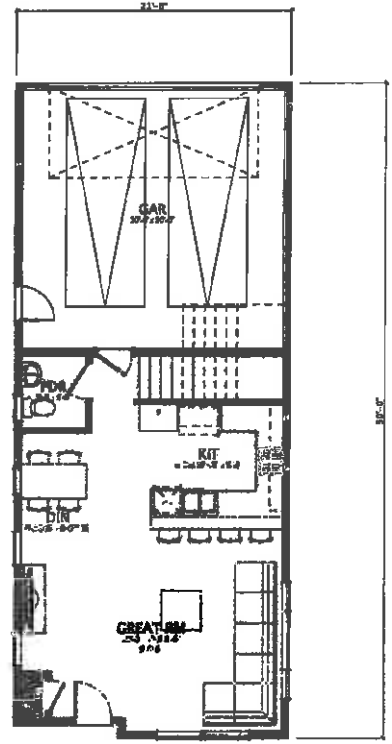
PLAN 1

SCALE NUMBER
A-11

Working Copy - Not for Construction - For Review Only - Not for Construction



② PLAN 2 SECOND FLOOR 885 sq ft



② PLAN 2 FIRST FLOOR 615 sq ft
 TOTAL 1500 sq ft
 2 BEDROOM, 2.5 BATH

KPI
 KNITTER PARTNERS
 INTERNATIONAL, INC.
 architecture & planning
 1762 AMARIL, NORTH, SUITE C
 IRRVING, CALIFORNIA 92614-5292
 949.762.1177 www.kpiinc.com

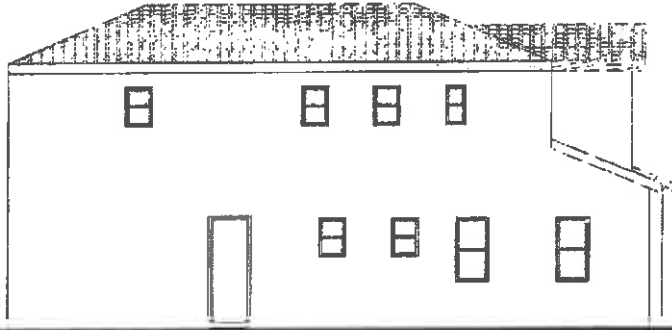
NO.	DATE	REVISION

Hengzhou Group
 177 E. Colorado Blvd Ste. 200
 Pasadena, CA 91105
 Gateway Heights
 Moreno Valley, USA

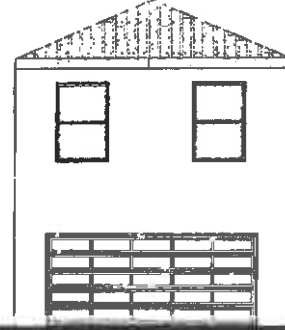
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PROJECT NAME/DATE:	NA
DRAWN BY:	JW
CHECKED BY:	ABC
SHEET TITLE	

PLAN 2

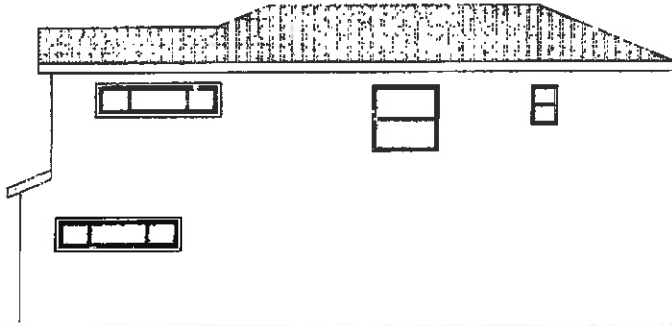
SHEET NUMBER
A-21



② PLAN 2 LEFT ELEVATION



② PLAN 2 REAR ELEVATION



② PLAN 2 RIGHT ELEVATION



② PLAN 2 FRONT ELEVATION

KPI
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 architecture & planning
 17752 Miragui, Norwalk, Suite C
 Norwalk, California 92851-4852
 949.752.1177 www.kpiinc.com
 Services provided by KPI International, Inc.

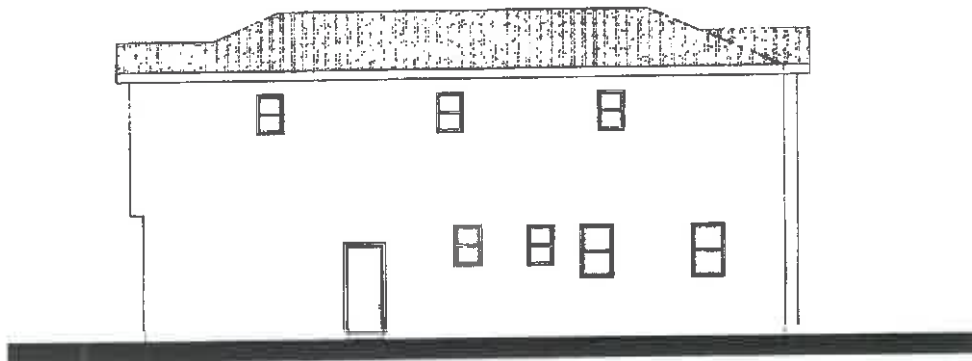
REV.	DATE	REVISION
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Hengzhou Group
 177 E. Colorado Blvd. Ste. 200
 Pasadena, CA 91104
 Gateway Heights
 Moreno Valley, USA

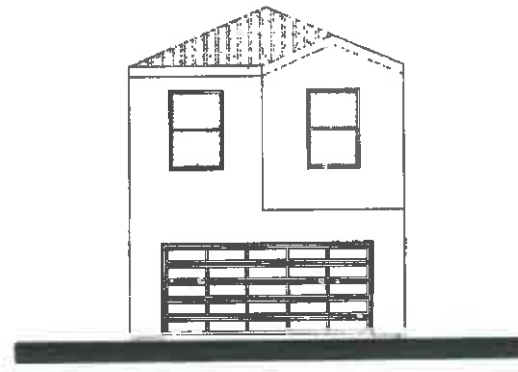
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PROJECT NUMBER	200
PROJECT NAME	14C
DRAWN BY	AW
CHECKED BY	AW
DATE	1/20/11

PLAN 2 EXTERIOR

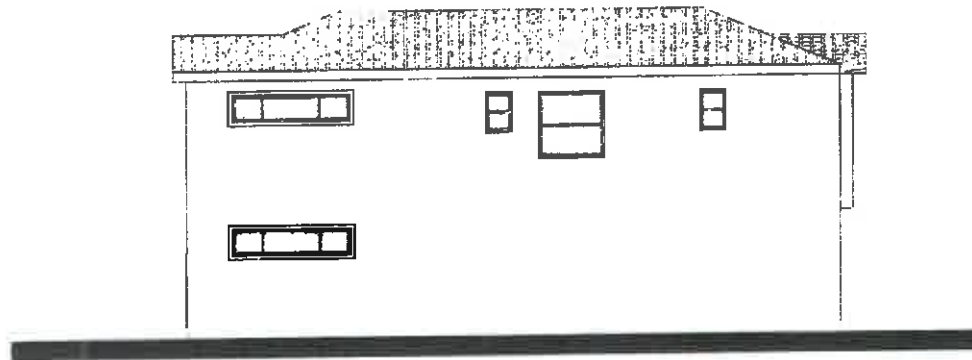
17752 Miragui, Norwalk, CA 92851-4852
 949.752.1177 www.kpiinc.com



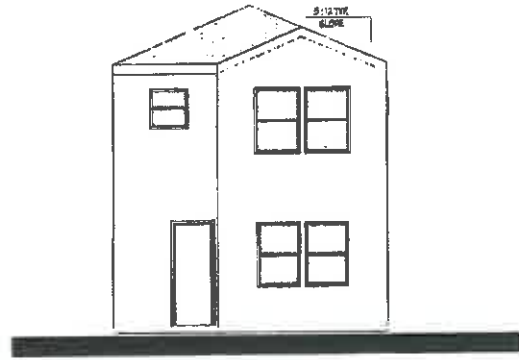
③ PLAN 3 LEFT ELEVATION



③ PLAN 3 REAR ELEVATION



④ PLAN 3 RIGHT ELEVATION



③ PLAN 3 FRONT ELEVATION

REV. DATE	REVISION
Δ	
Δ	
Δ	
Δ	
Δ	
Δ	
Δ	
Δ	

Henghou Group
 177 E. Colorado BLVD, Ste. 200
 Pasadena, CA 91105
Gateway Heights
 Moreno Valley, USA

PROJECT INFO	
PROJECT NUMBER	300
PROJECT MANAGER	FLC
DRAWN BY	JLV
SHEET ISSUE DATE	4/22
SHEET TITLE	

PLAN 3 EXTERIOR

SHEET NUMBER
A-32

Project: 177 E. Colorado Blvd, Ste. 200, Pasadena, CA 91105 - Gateway Heights - Moreno Valley, USA

PAGE BREAK





AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY

April 7, 2021

Mr. Russell Brady, Principal Planner
Riverside County Planning Division
4080 Lemon Street, 12th Floor
Riverside CA 92501

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County Administrative Center
4080 Lemon St., 12th Floor
Riverside, CA 92501
(951) 925-8132

www.aluc.org

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW –
DIRECTOR’S DETERMINATION**

File No.: ZAP1110FV21
Related File No.: PP26344 (Plot Plan Amendment)
APN: 480-170-019
Airport Zone: Zone D

Dear Mr. Brady:

As authorized by the Riverside County Airport Land Use Commission (ALUC) pursuant to its Resolution No. 2020-02, as ALUC Director, I have reviewed County of Riverside Case No. PP26344 (Plot Plan Amendment), a proposal to revise the internal floor plan for a 34,000 square foot Fitness Center (Building #11) as part of a 132,568 square foot retail commercial center on 21.16 acres which was previously found consistent under ZAP1087FV19, located on the northeast corner of Thompson Road and Highway 79 Winchester Road.

The proposed changes to the internal floor area of the 34,000 square foot fitness center building includes:

- 16,748 square feet of exercise room area,
- 1,020 square feet of office area,
- 105 square feet of reception/lounge area,
- 85 square feet of retail area,
- 2,146 square feet of daycare area,
- 1,308 square feet of locker room area,
- 1,916 square feet of pool area,
- 370 square feet of mechanical equipment room area, and
- 626 square feet of storage area.

The proposed revisions result in a total occupancy of 484 people, which results in an average intensity of 132 people per acre, and a single acre intensity of 484 people, both of which are consistent with Compatibility Zone D average and single acre criterion of 150 and 518 respectively. (Note, a 518 occupancy limit was based on the single-acre risk reduction measures that was granted to the original project for this building, warranting a 15% bonus to the normal single acre criterion, increasing it from 450 people to 518 people).

There are no changes to the footprint size of the building. The proposed revisions does not result in a change of occupancy (484 people) and intensity (132 people average acre and 518 people

AIRPORT LAND USE COMMISSION

single acre) from the original project ZAP1087FV19.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2007 French Valley Airport Land Use Compatibility Plan, as amended in 2011, provided that the County of Riverside applies the following recommended conditions:

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
2. The review of this Plot Plan is based on the proposed uses and activities noted in the project description. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, outdoor production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, critical community infrastructure facilities, highly noise sensitive outdoor nonresidential uses, and hazards to flight.
 - (f) Highly noise-sensitive outdoor nonresidential uses.
 - (g) Any use which results in a hazard to flight, including physical (e.g., tall objects), visual, and electronic forms of interference with the safety of aircraft operations.
3. The attached notice shall be provided to all prospective purchasers of the proposed parcels and tenants or lessees of the buildings, and shall be recorded as a deed notice.

AIRPORT LAND USE COMMISSION

4. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the stormwater basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

5. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission.
6. This project has been evaluated as consisting of 16,748 square feet of exercise room area, 1,020 square feet of office area, 105 square feet of reception/lounge area, 85 square feet of retail area, 2,146 square feet of daycare area, 1,308 square feet of locker room area, 1,916 square feet of pool area, 370 square feet of mechanical equipment room area, and 626 square feet of storage area. Any increase in building area, change in use to any higher intensity use, change in building location or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.
7. The maximum height of the building, including all roof-mounted equipment, if any, shall be limited to 43 feet, and the maximum top point elevation shall not exceed 1,408 feet above mean sea level unless a "Determination of No Hazard to Air Navigation" letter authorizing a higher top point elevation has been issued by the Federal Aviation Administration Obstruction Evaluation Service.

If you have any questions, please contact me at (951) 955-6893.

Sincerely,
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION



Paul Rull, ALUC Director

AIRPORT LAND USE COMMISSION

Attachments: Notice of Airport in Vicinity

**cc: French Valley Marketplace c/o Halferty Development Co. LLC (applicant/representative/
property owner)
Angela Jamison, County Airports Manager
ALUC Case File**

Y:\AIRPORT CASE FILES\March\ZAP1110FV21\ZAP1110FV21.LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b) (13)(A)

NOTICE

**THERE IS AN AIRPORT NEARBY.
THIS STORM WATER BASIN IS DESIGNED TO HOLD
STORM WATER FOR ONLY 48 HOURS AND
NOT TO ATTRACT BIRDS**

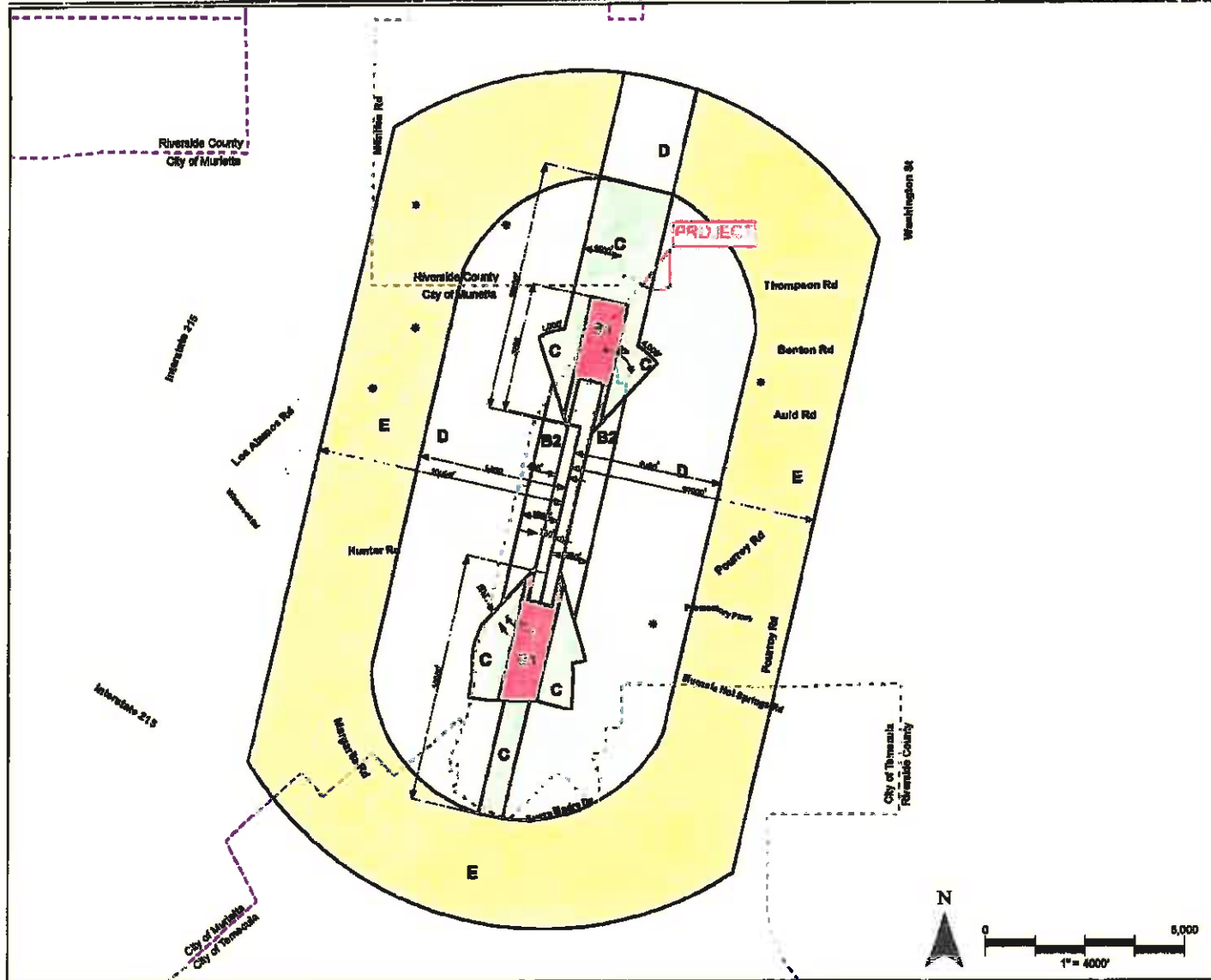
**PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES**



IF THIS BASIN IS OVERGROWN, PLEASE CONTACT:

Name: _____

Phone: _____



Legend

Compatibility Zones

- Airport Influence Area Boundary
- Zone A
- Zone B1
- Zone B2
- Zone C
- Zone D
- Zone E

Boundary Lines

- Airport Property Line
- - - City Limits
- * Height Review Overlay Zone

Note

Airport Influence Area boundary measured from a point 200 feet beyond runway ends in accordance with FAA airspace protection criteria (FAR Part 77). All other dimensions measured from runway ends and centerlines.

See Chapter 2, Table 2A from compatibility criteria associated with this map.

Riverside County
Airport Land Use Commission

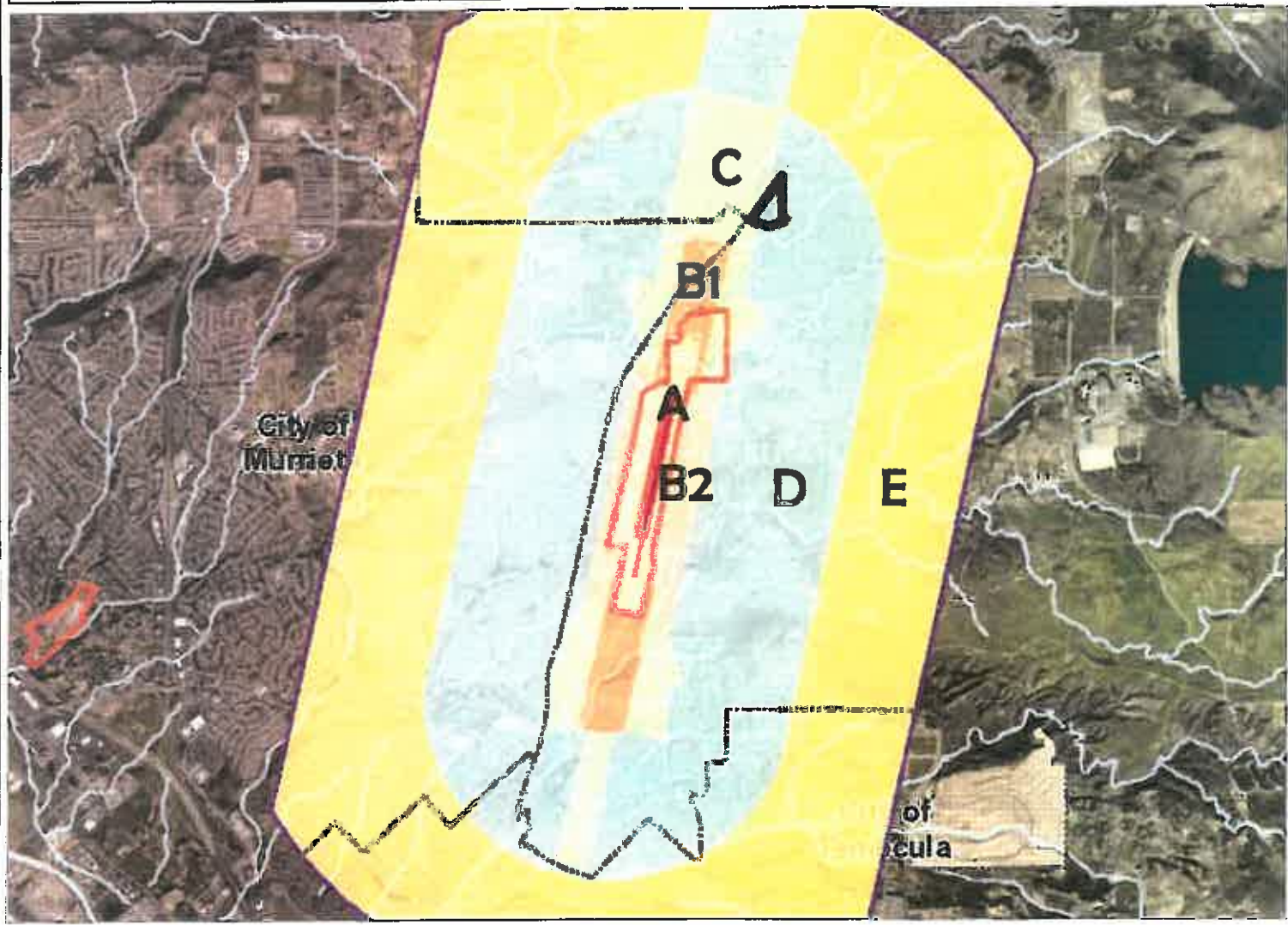
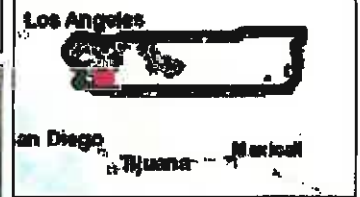
Riverside County
Airport Land Use Compatibility Plan
Policy Document

(April 2016)

Map FV-1

Compatibility Map
French Valley Airport

Map My County Map



- Legend**
- Runways
 - Airports
 - Airport Influence Areas
 - Airport Compatibility Zones
 - OTHER COMPATIBILITY ZONE
 - A
 - A-EXC1
 - B1
 - B1-APZ I
 - B1-APZ I-EXC1
 - B1-APZ II
 - B1-APZ II-EXC1
 - B1-EXC1
 - B2
 - B2-EXC1
 - C
 - C1
 - C1-EXC1
 - C1-EXC3
 - C1-EXC4
 - C1-HIGHT
 - C2
 - C2-EXC1
 - C2-EXC2
 - C2-EXC3
 - C2-EXC5
 - C2-EXC8



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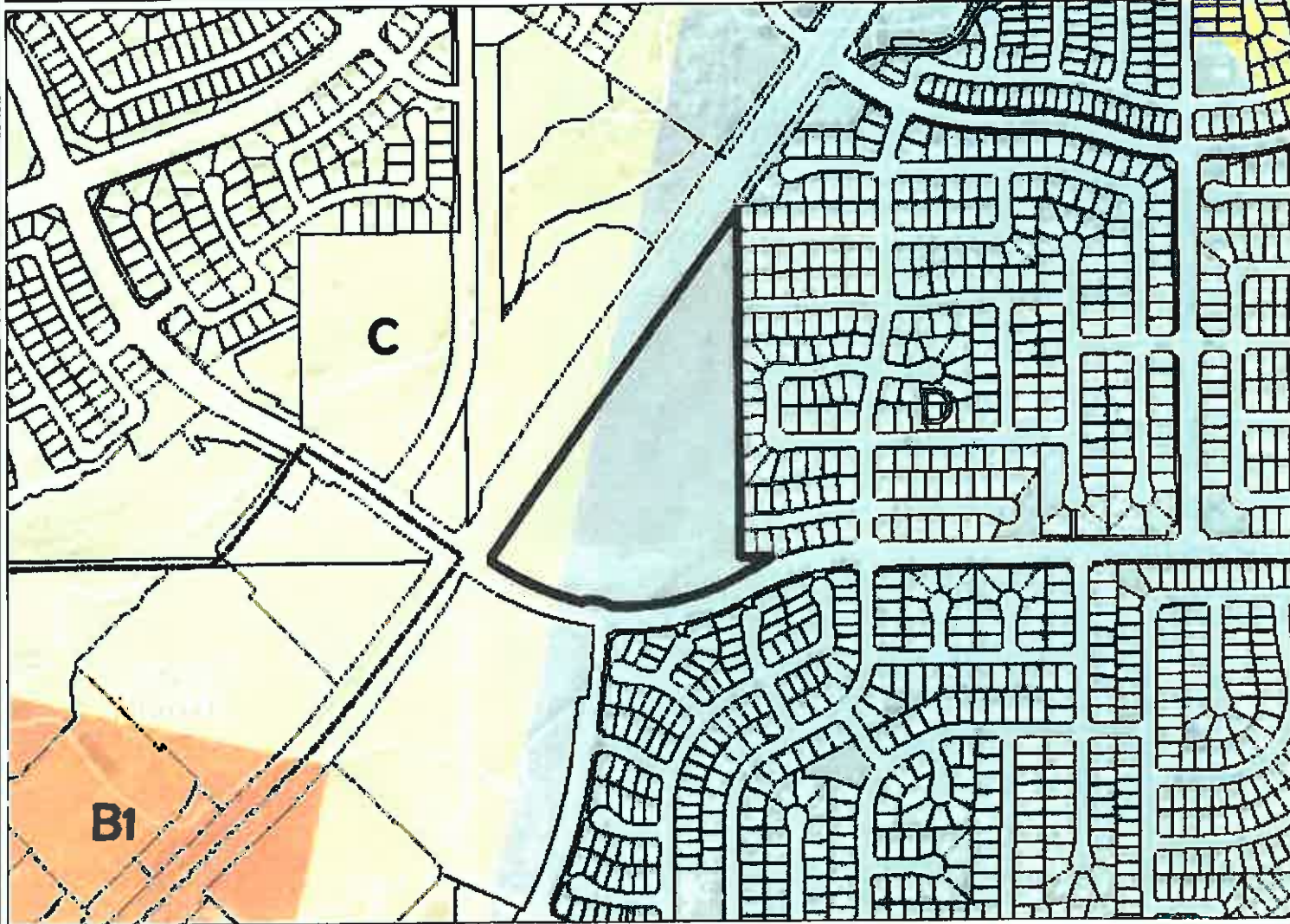


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Notes

Map My County Map



Legend

- Parcels
- Runways
- Airports
- Airport Influence Areas
- Airport Compatibility Zones**
- OTHER COMPATIBILITY ZONE**
- A
- A-EXC1
- B1
- B1-AFZ I
- B1-AFZ I-EXC1
- B1-AFZ II
- B1-AFZ II-EXC1
- B1-EXC1
- B2
- B2-EXC1
- C
- C1
- C1-EXC1
- C1-EXC3
- C1-EXC4
- C1-HIGHT
- C2
- C2-EXC1
- C2-EXC2
- C2-EXC3
- C2-EXC5



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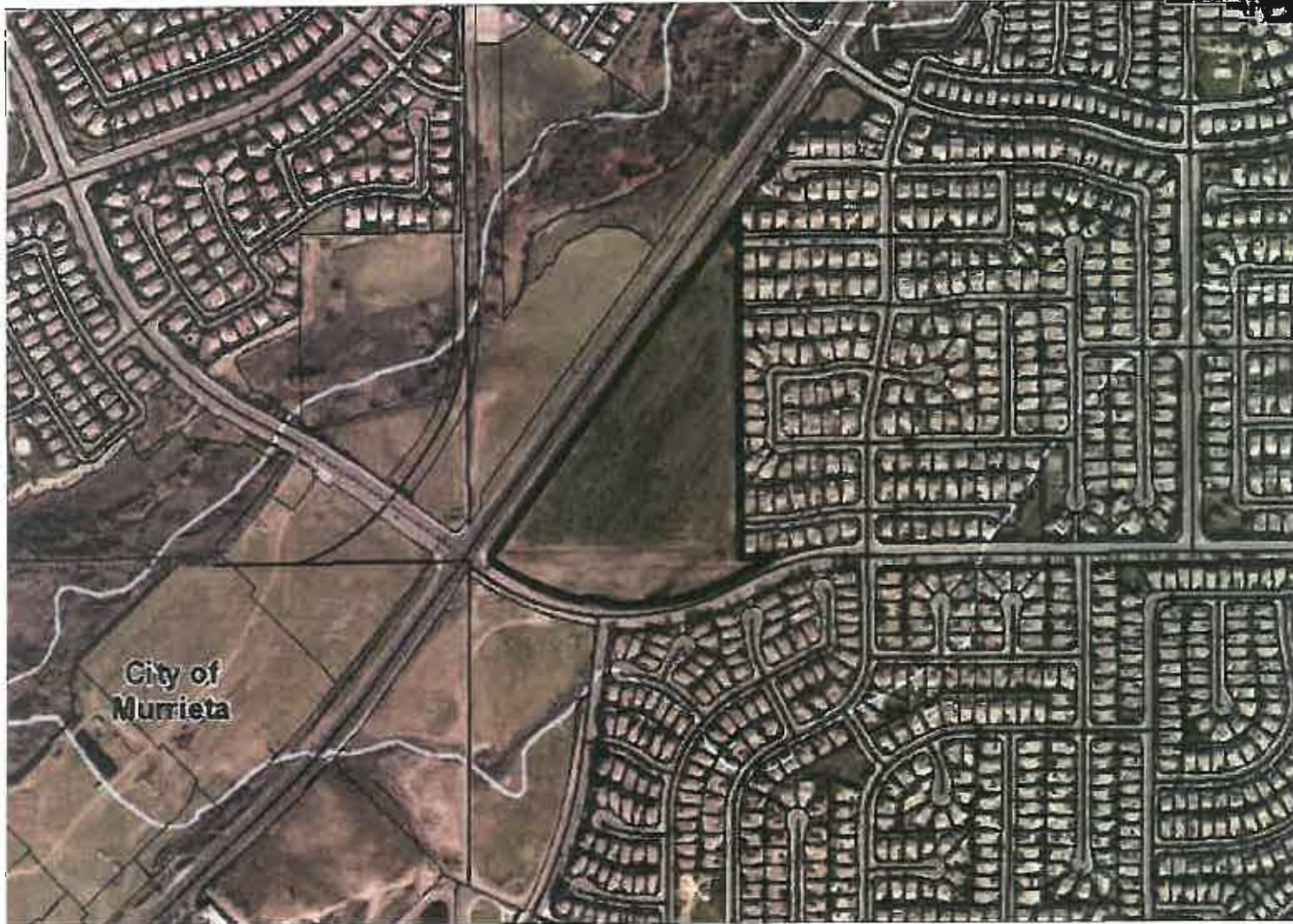
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Map My County Map



Legend

-  Parcels
-  County Centerlines
-  Blueline Streams
-  City Areas
-  World Street Map



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Notes

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Map My County Map



Legend

- County Centerlines
- Blue Line Streams
- City Areas
- World Street Map

Notes



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Legend

- County Centerlines
- Blue line Streams
- City Areas
- World Street Map



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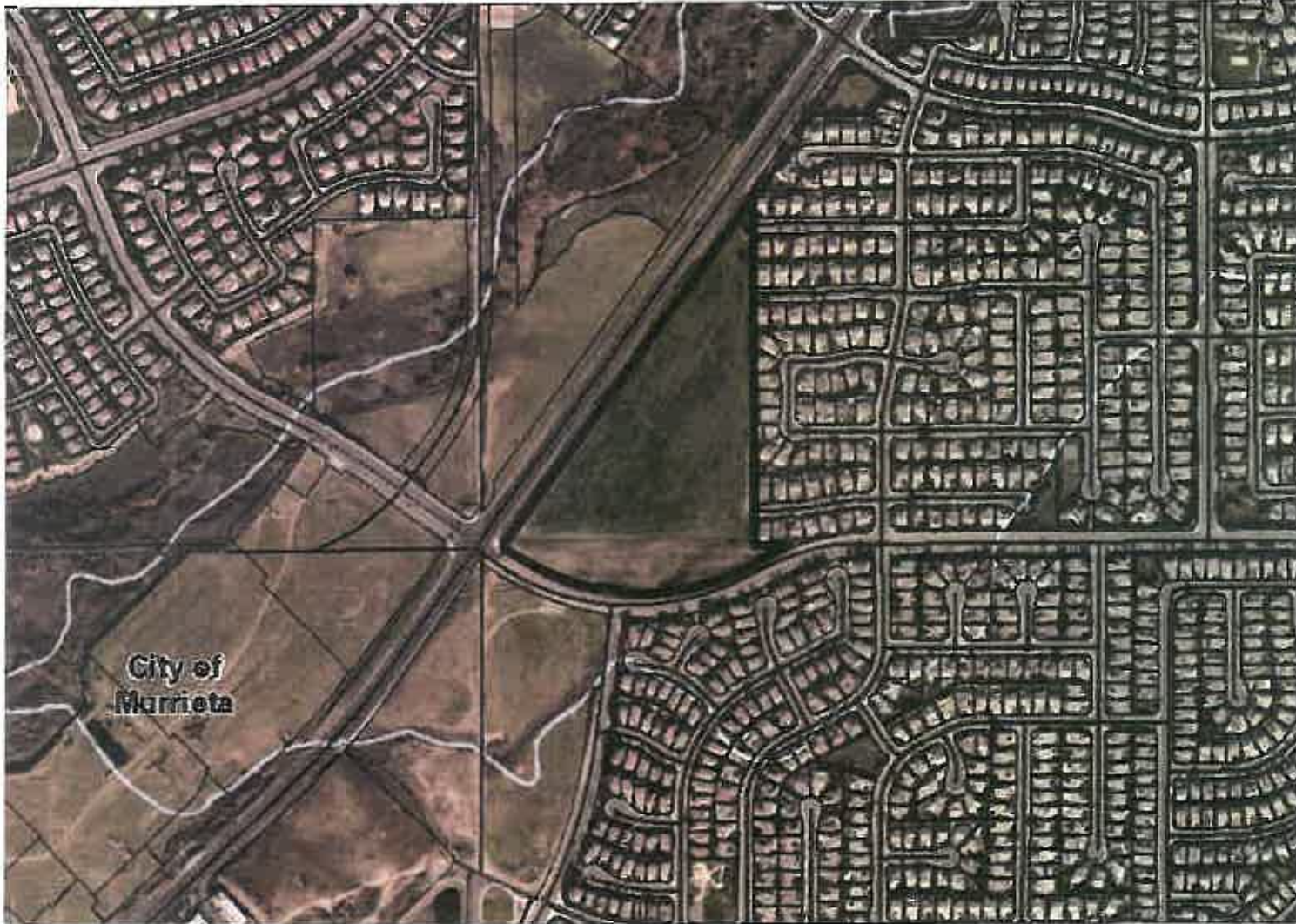
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


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Notes

Map My County Map



Legend

-  Parcels
-  County Centerlines
-  Blueline Streams
-  City Areas
-  World Street Map



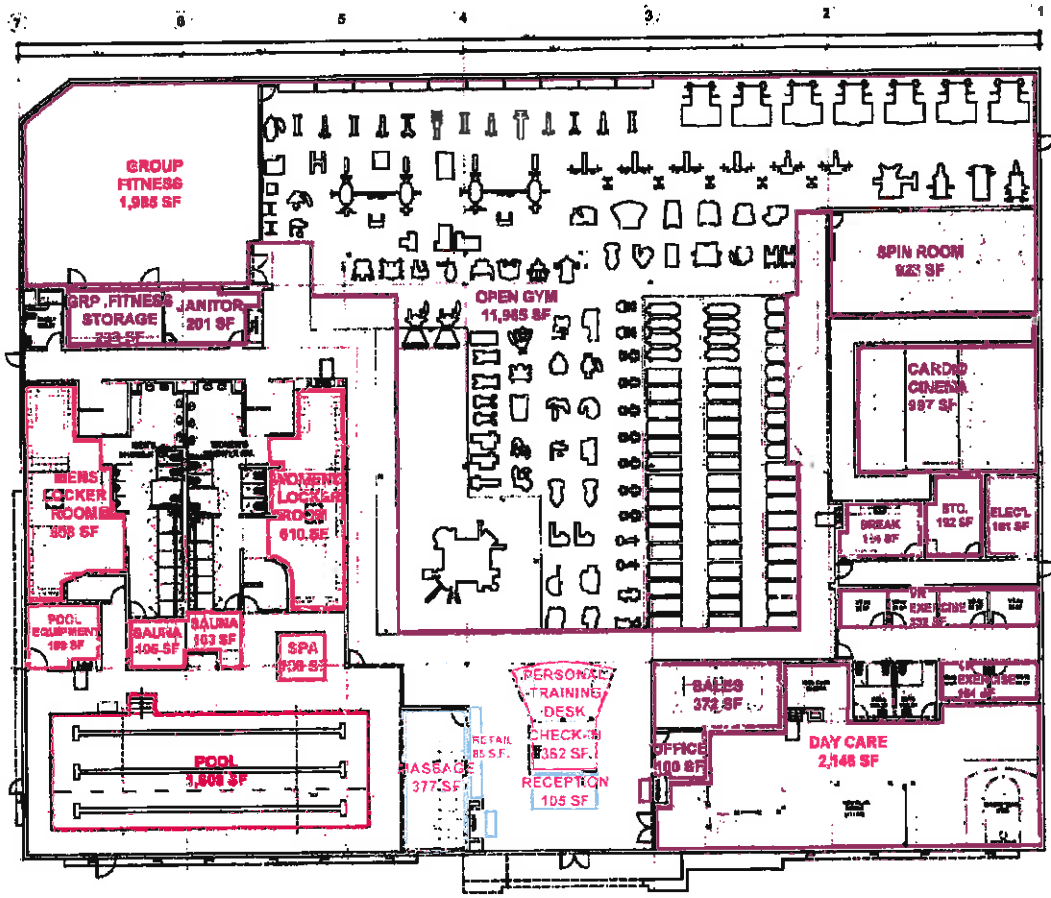
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

Notes

0 752 1,505 Feet

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USE	SIZE - S.F.	OCCUPANT LOAD FACTOR	OCCUPANTS
EXERCISE ROOMS	16,748 S.F.	1/50 SF	335 OCCU.
OFFICES	1,020 S.F.	1/200 SF	5 OCCU.
RECEPTION/LOUNGE	105 S.F.	1/15 SF	7 OCCU.
HL. TAIL	85 S.F.	1/20 SF	3 OCCU.
DAYCARE	2,148 S.F.	1/25 SF	82 OCCU.
LOCKER ROOMS	1,058 S.F.	1/50 SF	21 OCCU.
POOL	1,818 S.F.	1/20 SF	36 OCCU.
MECHL. EQUIPMENT ROOMS	370 S.F.	1/200 SF	2 OCCU.
STORAGE	628 S.F.	1/200 SF	3 OCCU.
TOTAL			484 OCCU.

**EOS - FRENCH VALLEY
RIVERSIDE, CA**

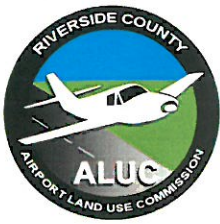
OCCUPANCY EXHIBIT

DATE: 02/16/2011
 DRAWING NO.: 10-0000-01
 PROJECT: EOS - FRENCH VALLEY
 SHEET: 1 OF 1
 DESIGNER: MCG ARCHITECTS
 CHECKED: MCG ARCHITECTS
 APPROVED: MCG ARCHITECTS
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**AIRPORT LAND USE COMMISSION MEETING
MINUTES
APRIL 8, 2021**

DRAFT

4-15-21

COMMISSIONERS PRESENT LIVE: Richard Stewart, Steven Stewart, John Lyon, Steve Manos, Arthur Butler (attended meeting until 11:15 am)

COMMISSIONERS PRESENT REMOTELY: Russell Betts, Gary Youmans

COMMISSIONERS ABSENT: None

2.0 PUBLIC HEARING: CONTINUED ITEMS

- 2.1 Staff report recommended: **CONTINUE to 5-13-21**
- Staff recommended at hearing: **CONTINUE to 5-13-21**
- ALUC Commission Action: **CONTINUED to 5-13-21 (Vote 7-0)**
- Motion: Richard Stewart**
Second: Steven Stewart
- ZAP1444MA20 – Jared Riemer/PR III/CHI Freeway BC, LLC (Representative: Glassman Planning Associates, MG2, and MIG, Inc.)** – March Joint Powers Authority Case No. PP14-02 (Plot Plan/Determination of Substantial Conformance No. 2). The applicant proposes to revise the occupancy use and floor plan of an existing (but vacant) 709,083 square foot high-cube industrial warehouse building to allow for a Delivery Parcel Hub facility on 39.42 acres, located southerly of Alessandro Boulevard, easterly of Interstate 215, westerly of Old 215 Frontage Road, and northerly of Cactus Avenue. There is no increase to the building's footprint. The building, as amended, would provide for 258,000 square feet of high-cube warehouse area, 8,000 square feet of office area, a 1,500 square foot break room, 126 loading/queuing van spaces, and 70 indoor van parking spaces. The original shell building project (ZAP1107MA14), which proposed 694,083 square feet of high-cube logistics warehouse, 12,000 square feet of first floor office area, and 3,000 square feet of second floor office mezzanine, was found consistent by ALUC in 2015. A subsequent revision to the high-cube warehouse building (ZAP1394MA19) was proposed to provide an additional 10,000 square feet of office area (by reducing warehouse area by the same square footage) was also found consistent by ALUC in 2020 (Airport Compatibility Zones B1-APZ-I and B1-APZ-II of the March Air Reserve Base/Inland Port Airport Influence Area). Continued from March 11, 2021. Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

3.0 PUBLIC HEARING: NEW CASES

- 3.1 Staff report recommended: **CONSISTENT**
- Staff recommended at hearing: **CONSISTENT**
- ALUC Commission Action: **CONSISTENT (Vote 7-0)**
- Motion: Steve Manos**
Second: Richard Stewart
- ZAP1093PS21 – AG Land Investments (Representative: Sanborn Architects)** – City of Palm Springs Case No. 5.1521 (General Plan Amendment, Change of Zone) and TTM38049 (Tentative Tract Map). The applicant proposes to divide 2.53 acres into 8 single-family residential lots located on the northeast corner of Alejo Road and Juanita Drive. The applicant also proposes to amend the site's General Plan land use designation from Industrial (IND) to Very Low Density Residential (VLDR), and change the site's zoning from Planned Research and Development Park (M-1-P) to Single-Family Residential (R-1-C) (Airport Compatibility Zone C of the Palm Springs International Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

VIDEO:

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**AIRPORT LAND USE COMMISSION MEETING
MINUTES
APRIL 8, 2021**

- 3.2 Staff report recommended: **CONSISTENT**
Staff recommended at hearing: **CONSISTENT**
ALUC Commission Action: **CONSISTENT (Vote 7-0)**
- Motion: Richard Stewart**
Second: Steve Manos
- 3.3 Staff report recommended: **CONDITIONALLY CONSISTENT**
Staff recommended at hearing: **CONDITIONALLY CONSISTENT** subject to the conditions included herein, and such conditions as may be required by the Federal Aviation Administration Obstruction Evaluation Service
ALUC Commission Action: **CONDITIONALLY CONSISTENT** subject to the conditions included herein, and such conditions as may be required by the Federal Aviation Administration Obstruction Evaluation Service **(Vote 7-0)**
- Motion: Richard Stewart**
Second: Steve Manos
- 3.4 Staff report recommended: **CONSISTENT**
Staff recommended at hearing: **CONSISTENT**
ALUC Commission Action: **CONSISTENT (Vote 7-0)**
- Motion: Steve Manos**
Second: Gary Youmans
- ZAP1109FV21 – Bear Creek Consulting Group, Inc. (Representative: Alan Long)** – County of Riverside Case No. CUP190048 (Conditional Use Permit). A proposal to construct a 2,500 square foot retail cannabis building on 1.19 acres, located easterly of Temeku Street, southerly of Benton Road, westerly of Penfield Lane, and northerly of Auld Road (Airport Compatibility Zone B1 of the French Valley Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org
- ZAP1108FV21 – David Beshay (Representative: Marks Architects, Gabriela Marks)** – County of Riverside Case No. PPT210004 (Plot Plan), TPM37990 (Tentative Parcel Map). A proposal to construct two separate restaurant buildings totaling 4,847 square feet with drive thrus on 3.68 acres located easterly of Winchester Road, southerly of Benton Road, westerly of Briggs Road, and northerly of Auld Road. The applicant also proposes to divide the site into two commercial parcels (Airport Compatibility Zones B1 and C of the French Valley Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org
- ZAP1454MA21 – LDC Industrial Realty LLC (Representative: HPA, LLC)** – City of Moreno Valley Case Nos. PEN20-0160 (General Plan Amendment), PEN20-0161 (Change of Zone), PEN20-0162 (Plot Plan), a proposal to construct a 164,489 square foot warehouse building with mezzanines on 8.06 acres, located on the northeast corner of Day Street and Alessandro Boulevard. The applicant also proposes amending the site’s General Plan land use designation from Residential: Max 30 du/ac (R30) to Business Park/Light Industrial (BP), and changing the site’s zoning from Residential: Max 30 du/ac (R30) to Light Industrial (LI) (Airport Compatibility Zone C1 of the March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

VIDEO:

2

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**AIRPORT LAND USE COMMISSION MEETING
MINUTES
APRIL 8, 2021**

- 3.5 Staff report recommended: **ZAP1456MA21 – Penske Truck Leasing Co., L.P/CD Moreno Holding LLC.** – City of Moreno Valley Case No. PEN21-0025 (Plot Plan) a proposal to construct a 21,155 square foot Penske Truck facility building and a 1,936 square foot truck fueling island on 9.10 acres located southerly of Alessandro Boulevard, easterly of Old 215 Frontage Road, and northerly of Cactus Avenue. Originally, a 16 building industrial business park (located partially on the proposed site) was proposed (ZAP1038MA7) and found consistent by the Commission in 2007. A revision to the project was proposed (ZAP1333MA18) that included 10 industrial warehouse buildings and a 15,280 square foot Penske Truck facility building, was found consistent by the Commission in 2018 (Airport Compatibility Zones B1-APZ-I and B1-APZ-II of the March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org
- CONTINUE to 5-13-21**
- Staff recommended at hearing: **CONTINUE to 5-13-21**
- ALUC Commission Action: **CONTINUED to 5-13-21 (Vote 7-0)**
- Motion: Richard Stewart
Second: Steve Manos**
- 3.6 Staff report recommended: **ZAP1452MA21 – OZI Alessandro, LLC (Representative: Matt Englhard)** – City of Riverside Case Nos. PR2020-000519 (General Plan Amendment, Specific Plan Amendment, Rezone, Design Review), a proposal to construct a 115,410 square foot warehouse building with mezzanine on 8.82 acres located northerly of Alessandro Boulevard, easterly of Interstate-215, westerly of Old 215 Frontage Road, and southerly of Cottonwood Avenue. The applicant also proposes amending 8.82 acres General Plan land use designation from Commercial (C) to Business/Office Park (B/OP), changing its zoning from CR-SP (Commercial Retail and Specific Plan ‘Sycamore Canyon Business Park’ Overlay Zones) to BMP-SP (Business and Manufacturing Park and Specific Plan ‘Sycamore Canyon Business Park Overlay Zones), and amending the Sycamore Canyon Business Park Specific Plan to change the land use from Industrial Support to Industrial. Originally, a mixed commercial-industrial project was proposed (ZAP1105MA14) and found inconsistent by the ALUC in 2015. A revised mixed commercial industrial project was proposed (ZAP1123MA15) and found consistent by the ALUC in 2015. (Airport Compatibility Zone B1-APZ-II of the March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org
- CONTINUE to 5-13-21**
- Staff recommended at hearing: **General Plan Amendment, Specific Plan Amendment, and Rezone be found CONSISTENT with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, and the Design Review also be found CONSISTENT, subject to the conditions included herein**
- ALUC Commission Action: **General Plan Amendment, Specific Plan Amendment, and Rezone be found CONSISTENT with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, and the Design Review also be found CONSISTENT, subject to the conditions included herein (Vote 7-0)**
- Motion: Steve Manos
Second: Richard Stewart**

VIDEO:

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**AIRPORT LAND USE COMMISSION MEETING
MINUTES
APRIL 8, 2021**

4.0 PUBLIC HEARING: MISCELLANEOUS ITEMS

4.1 Commissioner's Consideration to Adopt a Hybrid Intensity Policy for Parcel Delivery Facilities

Commissioner Manos motioned to continue the matter to May 13, 2021. Seconded by Richard Stewart. (Vote 6-0, absent Butler)

5.0 **ADMINISTRATIVE ITEMS**

5.1 Director's Approvals – Information only

5.2 Update March Air Reserve Base Compatibility Use Study (CUS)

Simon Housman, Project Director (MCUS) informed the Commission that we are still waiting for the Riverside County Board of Supervisors to approve the contract for the consultant. Once that is completed we will be moving into the next phase which is the meetings with the policy committee and working groups sometime next week.

5.3 Commissioner's Reappointments and Officer Selections for April Meeting

Both Commissioners Richard Stewart and Steven Stewart were reappointed to the Airport Land Use Commission for another four year term effective May 2021.

Commissioner Richard Stewart nominated Steven Stewart as the new Chair (Vote 6-0, absent Butler). Commissioner Lyon motioned to nominate Steve Manos as the new Vice Chair. Seconded by Steven Stewart. (Vote 6-0, absent Butler)

6.0 **APPROVAL OF MINUTES**

Commissioner Manos motioned to approve the March 11, 2021 minutes. Seconded by Steven Stewart. (Vote 6-0; Absent: Butler)

7.0 **ORAL COMMUNICATION ON ANY MATTER NOT ON THE AGENDA**

Paul Rull, ALUC Director reported that on Monday, April 5, 2021 the ALUC staff and Commissioners took a tour of the March Air Reserve Base and special thanks to Colonel Martin and Jamal Dada for coordinating the tour and accommodating us very well.

8.0 **COMMISSIONER'S COMMENTS**

Chair Betts expressed that he enjoyed serving as Chair for the past year and congratulations to the new Chair and Vice Chair.

9.0 **ADJOURNMENT**

Russell Betts, Chair adjourned the meeting at 11:26 a.m.

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