COUNTY OF RIVERSIDE AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM: 3.4 2.1

HEARING DATE: May 13, 2021 (Continued from March 11 April 8, 2021)

CASE NUMBER: ZAP1444MA20 – Jared Riemer/PR III/CHI Freeway BC,

LLC (Representative: Glassman Planning Associates, MG2,

and MIG. Inc.)

APPROVING JURISDICTION: March Joint Powers Authority

JURISDICTION CASE NO: PP14-02 (Plot Plan/Determination of Substantial

Conformance No. 2)

LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use

Compatibility Plan (March ALUCP)

Airport Influence Area: March Air Reserve Base

Land Use Policy: Compatibility Zones B1-APZ-I, B1-APZ-II

Noise Levels: 65-75 CNEL

MAJOR ISSUES: At the March 11, 2021, meeting, several new documents were submitted to the Commission. A detailed discussion of these documents is included in the staff report. Firstly, a multiple scenario intensity summary table was provided to the Commission by ALUC staff. The table identified several different scenarios on how to calculate the building's intensity for the Commission to consider in its decision-making process. Secondly, the applicant provided at the meeting a new floor plan increasing the breakroom area from 1,500 square feet to 6,500 square feet. The applicant also provided an occupancy survey study from an existing similar facility, with the intentions on applying the study's occupancies numbers for the proposed 6,500 square foot breakroom area, in lieu of using the Building Code to calculate the breakroom occupancy. This floor plan was subsequently redesigned later and replaced with a 5,400 square foot breakroom area. Lastly, the Air Force provided email comments dated March 9, 2021, expressing concerns with the enforceability of the Covenant, and requested the project be continued so they can review the Covenant further.

On April 1, 2021, the applicant submitted a revised site plan clarifying the breakroom area as being 5,800 square feet gross (previously identified as 5,400 square feet), and the plan was introduced at the April 8, 2021, meeting. The applicant subsequently revised the entire floor plan on April 20, 2021, providing for several smaller break room areas that are spaced 210 feet apart to avoid each others single acre area. This revised floor plan has been reviewed in this staff report

and is <u>consistent</u> with the average and single acre intensity criteria for B1-APZ-I and B1-APZ-II per the Building Code. As such, the submitted survey occupancy study is no longer needed.

In addition, Air Force comments were received on April 6, 2021, and introduced at the April 8 meeting. The Air Force comments identified that the use of covenants to restrict building occupation in projects are encouraged by the Air Force, with the ultimate goal of updating the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan (ALUCP) so that the intensity criteria is consistent with the Department of Defense Instruction No. 4165.57 intensity criteria.

The Commission also discussed the potential of adopting a Hybrid Intensity Policy which would allow staff to review Parcel Delivery project's intensity by using a hybrid method of the building code and parking code. Several concerns were brought up at the meeting about adopting such a policy, such as whether or not an amendment to the Compatibility Plan would be needed to take such an action. The item was continued to the May 13 meeting for further discussion and review.

The project was continued to the May 13, 2021 meeting, to allow ALUC staff time to review and analyze the new floor plan intensity, update the staff report, and review the potential adoption of the hybrid intensity policy.

Since the April 8 meeting, ALUC staff, has completed its review and analysis of the potential policy adoption and had the following comments:

- Section 2.5.2 of the 2011 California Airport Land Use Planning Handbook (Handbook), states that procedures that the ALUC will use in reviewing local plans and project should be defined in the ALUCP.
- The Handbook identifies several methods for determining usage intensities e.g. Building and Fire Codes, Facility Management Industry, Local Parking Standards, and Survey of Comparable Uses. The Handbook <u>does not</u> indicate or expressly allow the mixing of the various methods for determining usage intensities.
- Appendix C of the Countywide Policies only identify three methods of calculating intensity: Building Code, Parking Code, and Survey of Similar Uses. The Countywide Policies do not expressly allow the mixing or blending of these methods to determine concentration of people.
- In both the Handbook and Countywide Policies, examples of calculating occupancy shows these methodologies being <u>calculated independently</u> from each other.
- Because a 'Hybrid Intensity' type of calculation is not defined in the Countywide Policies, and because the Handbook does not specifically allow this type of calculation, it is Staff's position that such calculation would be <u>impermissible</u> without changing the Countywide Policies.
- Adopting such a policy would be an impermissible amendment to the ALUCP where the Handbook specifies the procedures for the ALUCP Amendments, under Section 2.4.2, and includes preparation of a draft ALUCP and environmental document, circulation for review and comment, noticing of a public hearing meeting, and adoption by resolution of the ALUCP and CEQA document.

Therefore, staff recommends that the Commission NOT adopt the Hybrid Intensity Policy for the above reasons and remove the matter off-calendar. By not adopting the policy, the proposed Amazon Parcel Delivery Hub project intensity would now be reviewed using the Building Code method and Parking Code method independently. Such an analysis has been included in this staff report. In addition, as a result of the applicant's recent changes to the floor plan, the project's average and single acre intensities are now consistent with the B1-APZ-I and B1-APZ-II criteria with using just the Building Code method, and therefore no longer needs the policy to be found consistent.

The applicant is proposing revisions to the floor plans (use areas) of previously reviewed and subsequently approved projects (ZAP1107MA14 and ZAP1394MA19) located within the portions of Compatibility Zones B1-APZ-I and B1-APZ-II. A breakdown of use by Compatibility Zone indicates that the project's average and single acre intensity in Compatibility Zone B1-APZ-I and B1-APZ-II is inconsistent with the intensity criteria when using the Building Code Method. (based on staff's hybrid intensity methodology combining the building code method and parking code method, and assigning an high-cube occupancy to unidentified floor areas which is now identified on the site plan. Any variation to this methodology may result in a changed determination). As such, the applicant has prepared a survey occupancy study from an existing similar facility for the Commission to consider. The study identified a highest hourly total of 45 people for the breakroom area, and when used in conjunction with the remaining 26 people for the high-cube warehouse area, the project (in B1-APZ-II) results in a single acre intensity of 71 people, which is consistent with B1-APZ-II intensity criteria of 100 people. This conclusion is based on the Commission's acceptance of the occupancy survey study.

Although the proposed project is consistent with the ALUC's Compatibility Zone B1-APZ-I and B1-APZ-II single acre intensity criteria, both single acre intensities in APZ-I and APZ-II exceeds the Air Force's interpretation of Air Force Instruction 32-7063 dated December 18, 2015, which addresses Air Force policies on Land Use Compatibility in accordance with Department of Defense Instruction (DoDI) No. 4165.57, for APZ-I and APZ-II. The Air Force understands the DoDI as limiting intensity to a maximum of 25 people in any given acre in APZ-II and a maximum of 50 people in any given acre in APZ-II.

In order to address this issue, the applicant has executed and recorded a Covenant on the title of the property, restricting actual occupancy of the building to a maximum of 25 people in any given acre in APZ-I, and a maximum of 50 people in any given acre in APZ-II. Operation in compliance with this covenant will be necessary to satisfy Air Force and March Joint Powers Authority concerns. regarding project intensity.

RECOMMENDATION: Staff recommends that the Commission find the Plot Plan/Determination of Substantial Conformance <u>CONSISTENT</u>, subject to the conditions included herein. Staff recommends that the Commission <u>CONTINUE</u> the matter to the April 8 May 13, 2021, meeting, pending completion of the Air Force review of the project.

PROJECT DESCRIPTION: The applicant proposes to revise the occupancy use and floor plan of an existing (but vacant) 709,083 square foot high-cube industrial warehouse building to allow for a delivery parcel hub facility on 39.42 acres. There is no increase to the building's footprint. The building, as amended, would provide for 258,000 307,000 691,805 square feet of high-cube warehouse area, 8,000 square feet of office area, 1,500 5,400 and 6,050 square feet of break room area (six separate areas). 126 loading/queuing van spaces, and 70 164 indoor van parking spaces.

On April 9, 2015, the Commission found the original project (ZAP1107MA14) consisting of 694,083 square feet of high-cube logistics warehouse, 12,000 square feet of first floor office area, and 3,000 square feet of second floor office mezzanine consistent with the March ALUCP. (That project also involved a General Plan Amendment and a Change of Zone.)

On January 9, 2020, the Commission found a subsequent revision to the original project (ZAP1394MA19) consisting of an additional 10,000 square feet of office area (by reducing warehouse area by the same square footage) also consistent with the March ALUCP.

PROJECT LOCATION: The site is located southerly of Alessandro Boulevard, easterly of Interstate 215, westerly of Old 215 Frontage Road, and northerly of Cactus Avenue within the land use jurisdiction of the March Joint Powers Authority, approximately 5,440 feet northwesterly of the northwesterly terminus of Runway 14-32 at March Air Reserve Base.

BACKGROUND:

Original Cases ZAP1107MA14 and ZAP1394MA19: The original project (ZAP1107MA14) proposed to construct a 709,083 square foot high-cube industrial warehouse building on 39.42 acres, and a subsequent revision (ZAP1394MA19) was proposed to revise the original building floor plan to provide for an additional 10,000 square feet of office area (by reducing warehouse area by the same square footage). Both projects were found consistent by the Commission.

It is important to note that both projects were consistent with the March Airport Land Use Compatibility intensity criteria, but also exceeded the intensity requirements for the Air Force's Department of Defense Instruction (DoDI) No. 4165.57 for APZ-I and APZ-II. This required the applicant to execute a Covenant, recorded on the title of the property, restricting the actual occupancy of the building to the DoDI intensity limits, which satisfied the Air Force and March Joint Powers Authority, and ultimately receiving ALUC consistency.

Multiple Scenario Intensity Summary Table: As introduced at the March 11, 2021, hearing, ALUC staff prepared a summary table which analyzed four different scenarios in calculating the building's intensity (table included in this staff report package). The intent of this table was to give the Commission an overview of the multiple way's building intensity could be calculated for this unique project.

After significant concerns were raised over the adoption of a Hybrid Intensity Policy at the April 8, 2021, meeting, such as whether or not an amendment to the Compatibility Plan would be needed to take such an action, ALUC staff conducted further research into the matter and had the

following comments:

- Section 2.5.2 of the 2011 California Airport Land Use Planning Handbook (Handbook), states that procedures that the ALUC will use in reviewing local plans and project should be defined in the ALUCP.
- The Handbook identifies several methods for determining usage intensities e.g. Building and Fire Codes, Facility Management Industry, Local Parking Standards, and Survey of Comparable Uses. The Handbook <u>does not</u> indicate or expressly allow the mixing of the various methods for determining usage intensities.
- Appendix C of the Countywide Policies only identify three methods of calculating intensity: Building Code, Parking Code, and Survey of Similar Uses. The Countywide Policies do not expressly allow the mixing or blending of these methods to determine concentration of people.
- In both the Handbook and Countywide Policies, examples of calculating occupancy shows these methodologies being <u>calculated independently</u> from each other.
- Because a 'Hybrid Intensity' type of calculation is not defined in the Countywide Policies, and because the Handbook does not specifically allow this type of calculation, it is Staff's position that such calculation would be <u>impermissible</u> without changing the Countywide Policies.
- Adopting such a policy would be an <u>impermissible</u> amendment to the ALUCP where the Handbook specifies the procedures for the ALUCP Amendments, under Section 2.4.2, and includes preparation of a draft ALUCP and environmental document, circulation for review and comment, noticing of a public hearing meeting, and adoption by resolution of the ALUCP and CEQA document.

Therefore, staff recommends that the Commission NOT adopt the Hybrid Intensity Policy for the above reasons and remove the matter off-calendar. By not adopting the policy, the proposed Amazon Parcel Delivery Hub project intensity would now be reviewed using the Building Code method and Parking Code method independently. Such an analysis has been included in this staff report. In addition, as a result of the applicant's recent changes to the floor plan, the project's average and single acre intensities are now consistent with the B1-APZ-I and B1-APZ-II criteria with using just the Building Code method.

In the March 11, 2021, staff report, ALUC staff analyzed the project under the "Hybrid Methodology" scenario, which used a combination of Building Code and Parking Code methods like applying an occupancy of 1.0 persons per van space instead of the standard 1.5 person per space. However, this method did not account for the unidentified areas of the building as shown on site plan.

Per the Building Code and direction from the County's Building Official, all floor areas are to be identified and assessed an occupancy. Therefore, the logic behind the "Hybrid Methodology 1: Unidentified Floor Area" scenario is the most applicable scenario for the project. This methodology uses the same parameters as the above-mentioned Hybrid Methodology, with the added inclusion of assigning a high-cube warehouse occupancy to the unidentified floor areas. As such, the applicant has revised their previous floor plan to show identifiable uses in these

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floor areas i.e. high-cube warehouse area and van parking spaces. These recent floor plans revisions are encapsulated in the new (fifth) ""Hybrid Methodology 2: All Floor Area Identified" in the table.

Non-Residential Land Use Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zones B1-APZ-I and APZ-II. Zone B1-APZ-I limits average intensity to 25 people per acre, and APZ-II limits average intensity to 50 people per acre. Approximately 29.15 acres are located within APZ-I and 12.8 acres within APZ-II.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan and the Additional Compatibility Policies included in the March ALUCP, the following rates were used to calculate the occupancy for the proposed project:

- High Cube Logistics Warehouse 1 person per 1,428 square feet,
- Office 1 person per 200 square feet, and
- Break Room − 1 person per 15 square feet,
- Delivery van loading spaces/queuing-spaces/van parking-spaces 1 person per vehicle (Although it is common practice to apply the standard of 1.5 persons per vehicle using the parking code method, these vehicles are specifically delivery vans with no passengers unlike cars, therefore, it is reasonable to apply a ratio of 1 person per vehicle).

The proposed use, a parcel delivery hub, is a unique land use that includes regular pre-defined floor area uses like office and warehouse, as well as indoor van parking spaces, and van stacking and queuing spaces (used to load merchandise from the warehouse area into the vans). This project uniqueness has prompted ALUC staff to use a hybrid methodology to calculate intensity; using the building code method for establish rooms/areas like office and warehouse, and using the parking code method to calculate the occupancy of the indoor van parking spaces, and van loading/queuing spaces.

The proposed amended project would provide for 258,000 307,000 691,805 square feet of high-cube warehouse area, 8,000 square feet of office area, 1,500 5,400 and 6,050 square feet of break room area (six separate areas), 126 loading/queuing van spaces, and 70 164 indoor van parking spaces, accommodating an occupancy of 518 905 928 people (which is exceeds the maximum allowed people in the building as specified in the existing recorded Covenant on the property), resulting in an average intensity of 13 24 people per acre for the entire site, which is consistent with the Compatibility Zone B1-APZ-I average intensity criterion of 25, and B1-APZ-II criterion of 50.

A breakdown of use by Compatibility Zone indicates that Zone B1-APZ-I includes 166,325

Zone B1-APZ-II includes 91,675 140,675 178,275 square feet of high-cube warehouse area, 8,000 square feet of office area, 1,500 5,400 and 2,900 square feet of break room area, and 18 loading/queuing van spaces 223 517 accommodating 358 people, resulting in an average intensity of 17 40 28 people per acre for the portion of the site located in Zone B1-APZ-II, which is consistent with the Compatibility Zone B1-APZ-II average acre intensity criterion of 50.

The project's average acre intensity is determined to be consistent, based on the hybrid methodology used by staff of combining the building code method and parking code method, as well as assigning a high-cube occupancy to unidentified floor areas now shown on the site plan. Any variation to this methodology may result in a changed determination.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per standard vehicle and 1.0 persons per van delivery and trailer truck in the absence of more precise data). Based on the number of parking spaces provided (354 standard vehicles, 21 truck trailer, 576 van outdoor parking), the total occupancy would be estimated at 1,128 people for an average intensity of approximately 29 people per acre for the entire site, which is consistent with the Compatibility Zone B1-APZ-II criterion of 50, but inconsistent with the Zone B1-APZ-I criterion of 25.

It is important to note that the building's proposed occupancy, as calculated by the Building Code, results in 518 905 928 people, which is significantly less than the occupancy as calculated by the Parking Code Method (1,128 people). Commensurate with the nature of a parcel delivery hub, 65% of the project's total parking spaces are designated for delivery vans, which will have off-setting shifts, never resulting in all the van parking spaces being occupied at the same time. Therefore, in this instance, it is more realistic that the building occupancy would be closer to the occupancy generated by the Building Code Method, rather than the occupancy generated by the Parking Code Method.

Non-Residential Single-Acre Land Use Intensity: Compatibility Zones B1-APZ-I and APZ-II limit maximum single-acre intensity to 100 people. There are no risk-reduction design bonuses available, as March Air Reserve Base/Inland Port Airport is primarily utilized by large aircraft weighing more than 12,500 pounds.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre intensity in APZ-I includes 42,510 square feet of high-cube warehouse area and 1,050 square feet of break room area, 52 56 indoor van parking spaces for a total occupancy of 52 56 100 people, which is consistent with the Compatibility Zone B1-APZ-I single acre intensity criterion of 100.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre intensity in APZ-II includes a 1,500 5,400 42,510 square feet of high-cube warehouse area and 1,050 square feet of break room area, 24,800 24,579 (previously unidentified floor area) for a total occupancy of 100 377 100 people, which is consistent with the Compatibility Zone B1-APZ-I

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single acre intensity criterion of 100.

The project's single acre intensity is determined to be inconsistent in Zone B1-APZ-II, based on the hybrid methodology used by staff of combining the building code method and parking code method, as well as assigning an high-cube occupancy to unidentified floor areas which is now shown on the site plan. Any variation to this methodology may result in a changed determination.

The project's average acre intensity is determined to be consistent, based on the hybrid methodology used by staff of combining the building code method and parking code method, as well as assigning a high-cube occupancy to unidentified floor areas. Any variation to this methodology may result in a changed determination.

Although the abovementioned single acre intensities in APZ-I and APAZ-II are is consistent with the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, both single acre intensities in APZ-I and APZ-II are inconsistent with the Air Force Department of Defense Instruction No. 4165.57 with regards to intensity, which is limited to a maximum of 25 people in any given acre in APZ-I, and 50 people in APZ-II. A more detailed analysis is provided below in the March Air Reserve Base section of the staff report.

Amazon Logistics – Occupancy Survey of Similar Facility: It is important to note that with the applicant's revised floor plans, the project's average and single acre intensities are now consistent with the B1-APZ-I and B1-APZ-II criteria using the Building Code method, and therefore no longer needs to submit the supplemental occupancy survey study for the Commission's consideration.

In light of the project's inconsistent single acre intensity (per the Building Code Method) in B1-APZ-II, the applicant prepared a survey study examining the occupancies of a similar parcel delivery facility.

The study highlights the fact that the building code method used for determining site intensity is a conservative approach in calculating actual project intensity. As such, the study examines one similar existing facility at 9350 Rayo Avenue, South Gate, for a more realistic occupancy generation calculation.

The study was taken between February 1, 2021, to March 2, 2021, and it identified the Rayo Avenue breakroom as having:

- highest daily total of 97 people,
- average daily total of 67 people, and
- highest hourly total of 45 people (taken from the survey days that could potentially exceed the single acre intensity criteria [when used in conjunction with the remaining 37,760 square feet of high-cube warehouse area accommodating 26 people], which includes data only available for one day February 1, February 7, February 8, February 11, February 12, and February 14).

Summary of Daily Counts: February 1st - March 2nd	_							
	1-Feb	97	9-Feb	65	17-Feb	67	25-Feb	69
	2-Feb	68	10-Feb	67	18-Feb	69	26-Feb	70
	3-Feb	73	11-Feb	76	19-Feb	66	27-Feb	68
	4-Feb	6Z	12-Feb	74	ZG-Feb	60	28-Féb	- 59
	5-Feb	51	13-Feb	71	21-Feb	69	1-Mar	61
	6-Feb	64	14 Feb	78	22-Feb	66	2-Mar	47
	7-Feb	78	15-Feb	63	23-Feb	55		
	8-Feb	74	16-Feb	60	24-Feb	73		

All of these results provide a significantly lower occupancy when compared to the proposed breakroom occupancy at the Alessandro Boulevard facility of 360 387 people, as calculated using the Building Code.

If we applied the highest hourly total from the study, 45 people, to the proposed 5,400 5,800 square foot breakroom at the Alessandro Boulevard facility, the project in the B1-APZ-II portion would accommodate a total occupancy of 590 212 people, More specifically, the intensity in B1-APZ-II would now result resulting in an average acre intensity of 18 17 people per acre, and a single acre intensity of 62 71 people (compared to 377 413 people using the Building Code method), both of which are consistent with the B1-APZ-II average and single acre intensity criteria of 50 and 100 respectively (there would be no changes to intensities in APZ-I).

The highest daily total of 97 people outlined in the survey study was not used in this ealeulation, as it does not accurately represent the highest number of people visiting the breakroom at any given time. Instead, it only identifies the total number of people entering the breakroom over the course of an entire day, and to use this number as an occupancy comparison would be unrealistic. Hourly occupancy data was provided, and it revealed a lower occupancy (per hour) than the daily total number.

It is important to note that the submitted survey study only counts the occupancy generated in the 4,720 square foot breakroom at the Rayo Avenue facility. It does not provide occupancy counts for the entire facility, which is what historically the Commission is accustom to. The applicant is requesting that the Commission considers applying the occupancy counted at the Rayo Avenue breakroom facility and apply it to the proposed project's breakroom on Alessandro Boulevard.

In addition, the building size at the Rayo Avenue facility (211,356 square feet) is 30% smaller than the size of the proposed facility at Alessandro Boulevard (709,083 square feet), which may be considered incomparable. Lastly, the study only provides an hourly breakdown for just one day (February 1) of the 30-day survey collection. There is a potential that the hourly totals for the other unaccounted days may be higher than 45 people.

March Air Reserve Base/United States Air Force Input: Given that the project site is located in Zones B1-APZ-I and APZ-II of the primary runway at March Air Reserve Base, the March Air Reserve Base staff was notified of the project and sent a package of plans for their review. The Air Force provided their previous 2015 comments regarding the original shell building project (see

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below). As of the time this staff report was prepared, we were still awaiting updated comments from the Air Force regarding this project. The Air Force letter dated March 10, 2015, included the following comments:

These comments have largely been addressed through the ALUC review and conditions. The only comment that is left unanswered is the Air Force's position on the proposed project with respect to permitted use and intensity i.e. if the Air Force accepts the existing executed Covenant as means of satisfying the AICUZ intensity criteria.

In addition, the Air Force provided email comments on March 9, 2021, outlining their concerns with the language of the Covenant agreement, specifically the enforceability of the Covenant regulations by the local jurisdictions. The Air Force has requested a continuance so they can review the Covenant and determine if any modifications are necessary.

The 2018 Airport Installation Compatible Use Zones (AICUZ) study identifies the project site as located within Accident Potential Zones I and II (APZ-I and APZ-II). Appendix A of the AICUZ provides Land Use Compatibility Tables for the APZs, which cite "warehousing" as permitted uses in APZ-I and APZ-II (and prohibited use in the Clear Zone [CZ]). The AICUZ does not explicitly identify a delivery parcel hub use.

However, March Air Reserve Base officials maintain that the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan is not consistent with current Air Force guidance found in Air Force Instruction 32-7063 dated December 18, 2015, which addresses Air Force policies on Land Use Compatibility in accordance with Department of Defense Instruction (DoDI) No. 4165.57. These inconsistencies include conflicts with regard to lot coverage, intensity, and permitted use definitions.

The proposed project complies with the restrictions on permitted uses and lot coverage, but not with the intensity limits. The Air Force understands the DoDI criteria as limiting intensity to a maximum of 25 people in any given acre in APZ-I and to a maximum of 50 people in any given acre in APZ-II. As noted above, the project would be expected to result in a single acre occupancy of 52 100 people in APZ-I and a single acre occupancy of 100 100 people in APZ-II.

The projected occupancy intensities would be inconsistent with the Air Force intensity understanding.

One method of bringing the project into consistency with both the Air Force Instruction (AFI) is for the applicant to agree to a condition including a Covenant, recorded on the title of the property, restricting the actual occupancy of the building to the limits of the AFI.

The **previous** applicant has agreed to this condition and has recorded and executed said document, which limits actual occupancy of the building in conformance with the limits of 25 and 50 persons, in any given acre within APZ-I and APZ-II, respectively. (The project's proposed occupancy of 518 people does not exceed the occupancy stated in the Covenant, and therefore the Covenant language is still applicable). Specifically, the existing Covenant states:

E. Covenanter has agreed to comply with the Density Restrictions and a Density Cap (both terms are defined below), by limiting occupancy of the Project to (i) five hundred and eighteen (518) occupants ("Density Cap") [THE DENSITY CAP WILL DECREASE IF THE SQUARE FOOTAGE OF THE BUILDING DECREASES.]; (ii) twenty-five (25) occupants in any square area measuring 208 feet by 208 feet ("Square Area") for all Square Areas within portions of the building of the Project within APZ I; and (iii) fifty (50) occupants in any Square Area within portions of the building of the Project within APZ II. Requirements (ii) and (iii) are collectively the "Density Restrictions", and are depicted in Exhibit B, attached hereto and incorporated herein by reference. Accordingly, any building expansion is prohibited, including an increase in the building mezzanine area, without further review by the JPA and MARB representatives, and consent and approval provided through an amendment to this covenant.

The applicant has agreed to a condition to revise the Covenant language which addresses the Air Force concerns, as well as limiting actual occupancy of the building to 25 persons in any given acre within APZ-II and to 50 people persons in any given acre within APZ-II.

The Air Force provided a comment letter dated April 6, 2021, which was submitted at the April 8, 2021, ALUC meeting. The letter identified that the use of the Covenant was encouraged by the Air Force, with the ultimate solution being the completion of the March Air Reserve Compatible Use Study and the subsequent update to the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, which addresses the intensity inconsistency between the 2014 March ALUCP and the Department of Defense Instruction No. 4165.57.

<u>Prohibited and Discouraged Uses:</u> The applicant does not propose any uses prohibited or discouraged in Compatibility Zones B1-APZ-I and APZ II. Industrial warehouse buildings are compatible within Accident Potential Zones I and II pursuant to the 2018 Air Installation Compatible Use Zone (AICUZ) study disseminated by the United States Air Force. The AICUZ does not explicitly identify a delivery parcel hub use. Use as an industrial warehouse is also compatible pursuant to Department of Defense Instruction (DODI) No. 4165.57, but the intensity levels of this project in the absence of the Covenant would exceed DODI allowances, as understood by the Air Force.

The Air Force previously had concerns with the original project regarding uncovered water in the detention basins being a bird attractant source. Hazards to flight are prohibited in Compatibility Zones B1-APZ-I and APZ-II. However, these concerns were addressed with special ALUC drainage conditions designed to minimize the potential for the proposed basins to become bird attractants.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being in an area subject to aircraft noise in the 65-70 CNEL range. As a primarily industrial use not sensitive to noise (and considering typical anticipated building construction noise attenuation of approximately 20 dBA), the warehouse area would not require special measures to mitigate aircraft-generated noise. However, a condition is included to provide for adequate noise attenuation within office areas of the building so as to achieve an interior noise level of 45 CNEL.

Part 77: The elevation of Runway 14-32 at its northerly terminus is approximately 1535.1 feet above mean sea level (1,535.1 feet AMSL). At a distance of approximately 5,920 feet from the runway, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,594.3 feet AMSL. The apparent finished floor elevation of the building is approximately 1,541 feet AMSL. The proposed building has a maximum height of 44.3 feet for a potential maximum elevation of 1,585.3 feet AMSL. Therefore, review by the FAA Obstruction Evaluation Service would not normally be required. However, March Joint Powers Authority, the jurisdiction of record, requires submittal of Form 7460-1 for all building projects within their area. The original applicant submitted Form 7460-1, the FAA assigned Aeronautical Study No. 2015-AWP-566-OE, and a Determination of No Hazard letter was issued by the FAA OES on May 29, 2015. A new submittal (2018-AWP-11013-OE) was made in 2018, and FAA issued a Determination of No Hazard letter on July 16, 2018.

The proposed floor plan change does not alter the building height or the FAA's no hazard determination.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

CONDITIONS:

- 1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
- 2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)

- (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
- (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, congregate care facilities, hotels/motels, places of assembly, restaurants, hazardous materials manufacture/storage (excluding storage of quantities of less than 6,000 gallons of flammable materials in the APZ II portion of the property), noise sensitive outdoor nonresidential uses, and hazards to flight.
- (f) Retail trade, eating and drinking establishments, personal services, professional services, educational services, governmental services, medical facilities, cultural activities, and any other uses providing on-site services to the public.
- (g) Commercial service uses; civic uses; churches, chapels, and other places of worship; classrooms; gymnasiums; theaters; conference or convention halls; auditoriums; fraternal lodges; gaming; auction rooms.
- (h) Manufacturing of: food and kindred products, textile mill products, apparel, chemicals and allied products, rubber and plastic products, fabricated metal products, professional, scientific, and controlling instruments, photographic and optical goods, watches and clocks.
- 3. Prior to issuance of any building permits, the landowner shall convey and have recorded an avigation easement to the March Inland Port Airport Authority. Contact March Joint Powers Authority at (951) 656-7000 for additional information.
- 4. The attached notice shall be given to all prospective purchasers of the property and/or tenants of the building. While not required, the applicant and its successors-in-interest are encouraged to provide a copy of said notice to employees who would regularly be working at this location.
- 5. Any new detention basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the detention basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

- 6. In order to ensure proper functioning of the project drain system to avoid potential hazards to March Air Reserve Base flights, an additional Best Management Practice (BMP) shall be added to the project Water Quality Management Plan (WQMP). The applicant shall enter into a covenant and agreement with the March Joint Powers Authority similar to the Water Quality Management Plan and Urban Runoff BMP Transfer, Access and Maintenance Agreement between March Joint Powers Authority and Sun Life Assurance Company of Canada (Document No. 2014-0030862), which shall be recorded prior to issuance of a certificate of occupancy. A copy of the recorded agreement and BMP shall be provided to the Riverside County Airport Land Use Commission. The BMP shall include the following program:
 - a. The property owner (Proficiency 215 LLC or its successor(s)-in-interest, hereinafter "Owner") or its designated representative shall monitor the conditions of the detention basins and promptly inspect such basins following the completion of each "significant" rain event and the 48-hour period thereafter.
 - b. If any standing water remains in a basin that is not beneath a rock, gravel, or other layer following the completion of the "significant" rain event and the 48 hour period thereafter, Owner or its designated representative shall arrange to have such standing water either removed or covered within the next two business days following the conclusion of the 48 hour period.
 - c. In the event that the standing water situation recurs on a regular basis following the 48-hour detention period, the detention basin may no longer be draining as originally designed to prevent standing water from rising above a rock, gravel or other layer (for example, due to a rise in groundwater levels or other circumstance beyond Owner's ability to control). In that situation, Owner or its designated representative shall promptly engage a licensed civil engineer to prepare a design plan to assure that such condition does not persist for more than 48 hours following the conclusion of a "significant" rain event. The required engineering design solution shall be implemented promptly, but no later than 180 days following its approval by all applicable authorities, providing that, until such time as the engineered design solution is implemented, Owner or its designated representative will maintain water levels below the rock, gravel, or other layer.

(As amended by the Airport Land Use Commission on April 9, 2015)

- 7. This project has been evaluated as a proposal for 258,000 307,000 691,805 square feet of high-cube warehouse area, 8,000 square feet of office area, 1,500 5,400 and 6,050 square feet break room area (six separate areas). 126 loading/queuing van spaces, and 70 164 indoor van parking spaces. Any increase in building area, change in use to any higher intensity use, change in building location, or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.
- 8. Zoned fire sprinkler systems shall be required throughout the building.
- 9. Office space must have sound attenuation features sufficient to reduce interior noise levels from exterior aviation-related sources to no more than CNEL 45 dB. March Joint Powers Authority shall require an acoustical study to ensure compliance with this requirement.
- 10. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
- 11. The project shall be in compliance with the recorded and executed Covenant, which limits building occupancy to a maximum of 25 people in any given acre in APZ-I and 50 people in any given acre in APZ-II.
- 12. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission and March Air Reserve Base.
- 13. The Federal Aviation Administration has conducted an aeronautical study of the proposed structure (Aeronautical Study No. 2018-AWP-11013-OE) and has determined that neither marking nor lighting of the structure is necessary for aviation safety. However, if marking and/or lighting for aviation safety are accomplished on a voluntary basis, such marking and/or lighting (if any) shall be installed in accordance with FAA Advisory Circular 70/7460-1 L Change 1 and shall be maintained in accordance therewith for the life of the project.
- 14. The maximum height of the proposed structure (including any roof-mounted equipment) shall not exceed 48 feet above ground level, and the maximum elevation of the proposed structure at top point shall not exceed 1,585 feet above mean sea level.
- 15. The specific coordinates, height, and top point elevation of the proposed structure shall not be amended without further review by the Airport Land Use Commission and the Federal Aviation Administration; provided, however, that reduction in structure height or elevation shall not require further review by the Airport Land Use Commission.

- 16. Temporary construction equipment used during actual construction of the proposed structure shall not exceed the height of the structure (48 feet), unless separate notice is provided to the Federal Aviation Administration through the Form 7460-1 process.
- Within five (5) days after construction of the proposed structure reaches its greatest height, FAA Form 7460-2 (Part II), Notice of Actual Construction or Alteration, shall be completed by the project proponent or his/her designee and e-filed with the Federal Aviation Administration. (Go to https://oeaaa.faa.gov for instructions.)
- 18. The applicant has agreed to accept a Covenant which will be recorded on the title of the property restricting the actual occupancy of the buildings to the limits of the Air Force Instruction, and to the satisfaction of the Air Force. The project shall be incompliance with the recorded and executed Covenant, which limits building occupancy to a maximum of 25 people in any given acre in APZ-I, and a maximum of 50 people in any given acre in APZ-II. The Covenant shall include the following language:

"Covenanter has agreed to comply with the Density Restrictions and a Density Cap (both terms are defined below), by limiting occupancy of the Project to (i) five hundred and eighteen (518) occupants ("Density Cap") [THE DENSITY CAP WILL DECREASE IF THE SQUARE FOOTAGE OF THE BUILDING DECREASES.]; (ii) twenty-five (25) occupants in any square area measuring 208 feet by 208 feet ("Square Area") for all Square Areas within portions of the building of the Project within APZ I; and (iii) fifty (50) occupants in any Square Area within portions of the building of the Project within APZ II. Requirements (ii) and (iii) are collectively the "Density Restrictions", and are depicted in Exhibit B, attached hereto and incorporated herein by reference. Accordingly, any building expansion is prohibited, including an increase in the building mezzanine area, without further review by the JPA and MARB representatives, and consent and approval provided through an amendment to this covenant."

Compliance shall be verified by MJPA or third-party inspections and reports on a schedule agreed upon by the applicant/project operator, the MJPA, and MARB representatives.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b)

NOTICE

THERE IS AN AIRPORT NEARBY.

THIS STORM WATER BASIN IS DESIGNED TO HOLD STORM WATER FOR ONLY 48 HOURS AND NOT TO ATTRACT BIRDS

PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES



IF	THIS	BASIN	15	OVERGROWN,	P	LEASE	CONTACT
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Name:		Phone:	2	
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DEPARTMENT OF THE AIR FORCE AIR FORCE RESERVE COMMAND

April 6, 2021

Colonel Rodney E. McCraine Commander 452d Mission Support Group 1261 Graeber Street, Bldg. 2313, Suite 135 March ARB, CA 92518

Mr. Paul Rull Director Riverside County Airport Land Use Commission 4080 Lemon Street, 14th Floor Riverside, CA 92501

Dear Mr. Paul Rull,

This is in response to three applications, listed below, being considered by the Riverside County Airport Land Use Commission (ALUC) and addresses the application of Department of Defense Instruction (DODI) 4165.57 and Office Density Restriction Covenants for Riverside County Airport Land Use Commission (ALUC) on these cases –

- ZAP1444MA20 (Amazon Parcel Delivery Hub at Freeway Business Center, March JPA),
- ZAP1452MA21 (OZI Warehouse, City of Riverside),
- ZAP1456MA21 (Penske Truck Facility, City of Moreno Valley)

With respect to the 3 projects, and for the any future projects and applications, March ARB encourages the ALUC to follow the DODI 4165.57 as a minimum guide to determining office density restrictions and appropriate covenants to be used in ensuring that the density restrictions are met. The Instruction encourages local governments to address labor intensity, which addresses the allowable number of persons within a building and even in portions of the buildings. The most recent F-16 aircraft accident in March 2019 near March ARB is an example of the risk to property and life that exists when developments encroach upon an Air Base. Luckily, the F-16 crashed in a portion of the building with very low labor intensity and no one was injured. However, the accident could have easily happened only a few feet to the west where the higher labor intensity of the office space in that building could have resulted in serious bodily injury or death.

The ALUC should always consider the risk of an aircraft accident when making a decision. The use of Accident Potential Zones (APZ) described in the DODI 4165.57 address the general risk, but the DODI is only minimal guidance for use by local authorities to reduce the risk to the general public. There are two equally important aspects to the risk: the potential for aircraft to have an accident, and the potential for the public to be impacted by an accident. The Air Force (AF) is ready to work with ALUC to address March ARB risks and approaches to reduce the potential for accidental injury or death to the public both in and outside of allowable developments within the APZs. It is the Air Forces desire that the ALUC would side with conservative limits on building and development occupations.

The ALUC and local governments can do much to reduce potential accidents by having strong codes to reduce the cumulative development impact on air-pollution, electronic interference with aircraft, height of structures, and potential glare to pilots. Likewise, ALUC and local governments can greatly reduce the potential risk to the public by strong codes to reduce occupation density of land uses within the APZs, not only within certain land uses, but also labor intensity within building uses in the APZs.

ALUC's use of deed covenants to restrict building occupation in developments, which are not located in a city with adequate land use controls, is encouraged by the AF. The specific wording must ensure that the entity having jurisdiction be able to enforce the covenant. The AF is not able to inspect or enforce any deed covenants. This is a local government responsibility and authority. Covenants are not easily enforced and strict land use controls by the City and County are the real solution to not only reduce the risk but to enforce the reduced risk in the future.

Finally the AF will work with the ALUC in its Compatibility Use Study to develop a Compatibility Use Plan to develop further guidance for local governments to address and enforce; occupation density in APZs, labor intensity within buildings and other factors, to include the cumulative impacts of multiple building solar glare, air emissions, bird and wildlife hazards, etc. We look forward to working with the ALUC and its member governments in this regard.

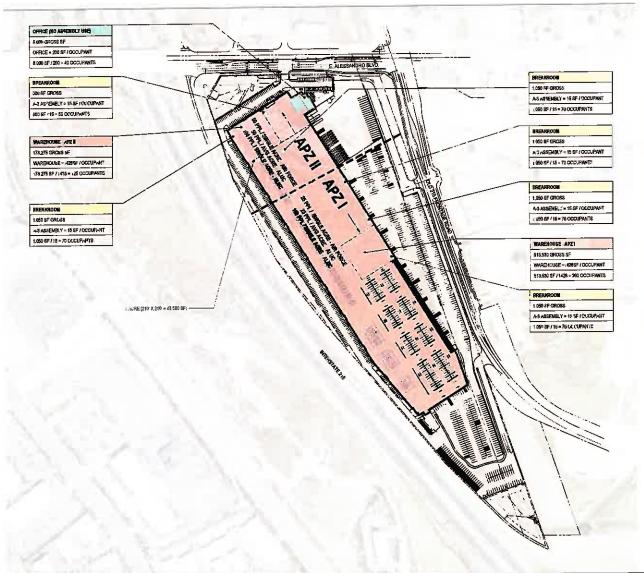
Should you have any questions or would like to discuss this further, please feel free to contact me directly at (951) 655-3700.

Sincerely

MCCRAINE.RO Digitally signed by DNEY.ERIC.10 C.1067944492 67944492

MCCRAINE.RODNEY.ERI Date: 2021.04.05 15:55:27 -07'00'

RODNEY E. MCCRAINE, Colonel, USAF Commander, 452d Mission Support Group



PARKING BREAKDOWN

TOTAL ASSOCIATE STALLS (18' x 9')	340
TOTAL ACCESSIBLE PARKING STALLS	14
VAN PARKING (EXTERIOR)	576
VAN PARKING (INTERIOR)	70
TOTAL VAN PARKING STALLS (27' x 11')	646

OCCUPANCY BREAKDOWN

	APZ I (29.15 AC)			PZ ,8 AC)	TOTAL		
	AREA (SF)	OCCUPANTS	AREA (SF)	OCCUPANTS	AREA (SF)	OCCUPANTS	
OFFICE	-	-	8,000	40	9,000	40	
BREAK ROOM	3,150	210	2,900	193	8,050	403	
WAREHOUSE (@ 1428 SF /PERSON)	513 530	360	178,275	125	691,805	485	
TOTAL	516,680	570	189,175	358	705,855	928	

AVG. PER ACRE (25 APZI / 50 APZII)	COMPLIES	19.6	COMPLIES	28
MAX SINGLE ACRE OCCUPANTS (100)	COMPLIES	100	COMPLIES	100



Architect contact: Kevin Marx Phone; 206,962.6483 Email: Kevin,Marx@mg2.com

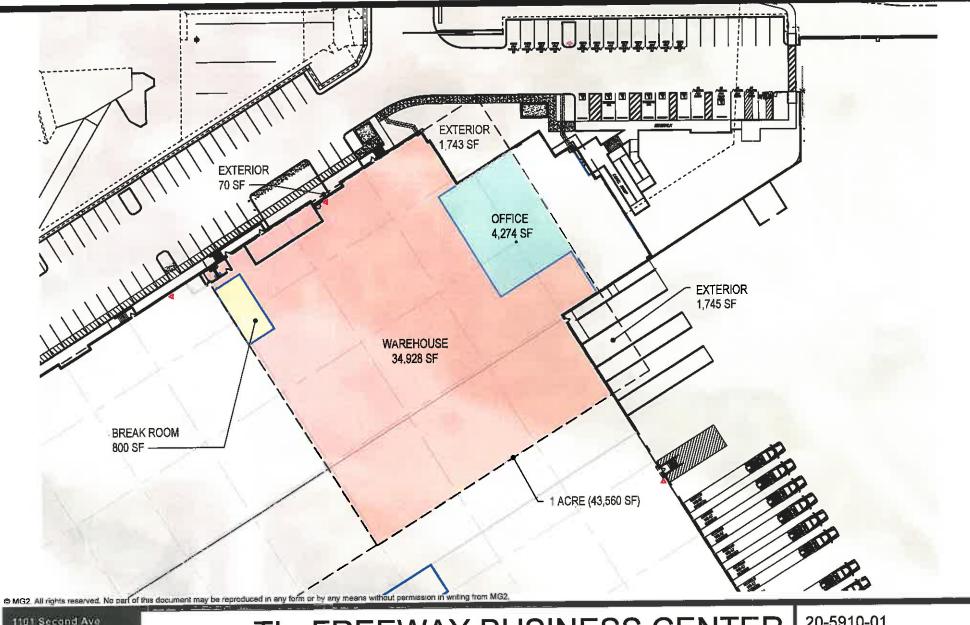


1" = 150'-0"

TI - FREEWAY BUSINESS CENTER

2677 E Alessandro Blvd Riverside, CA 92508 A P R I L 1 9, 2 0 2 1

SITE PLAN



1101 Second Ave Ste 100 Seattle, WA 98101 206 962 6500 MG2.com

TI - FREEWAY BUSINESS CENTER

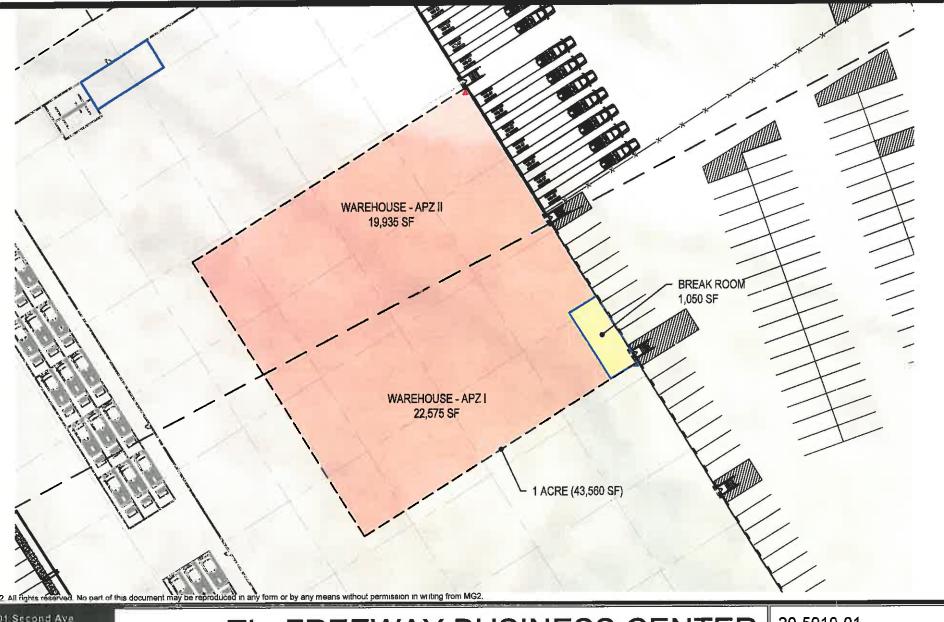
2677 E Alessandro Blvd Riverside, CA 92508

SINGLE ACRE DIAGRAM

20-5910-01

PM: KM

April 20, 2021



1101 Second Ave Ste 100 Seattle, WA 98101 206 962 6500 MG2.com



TI - FREEWAY BUSINESS CENTER

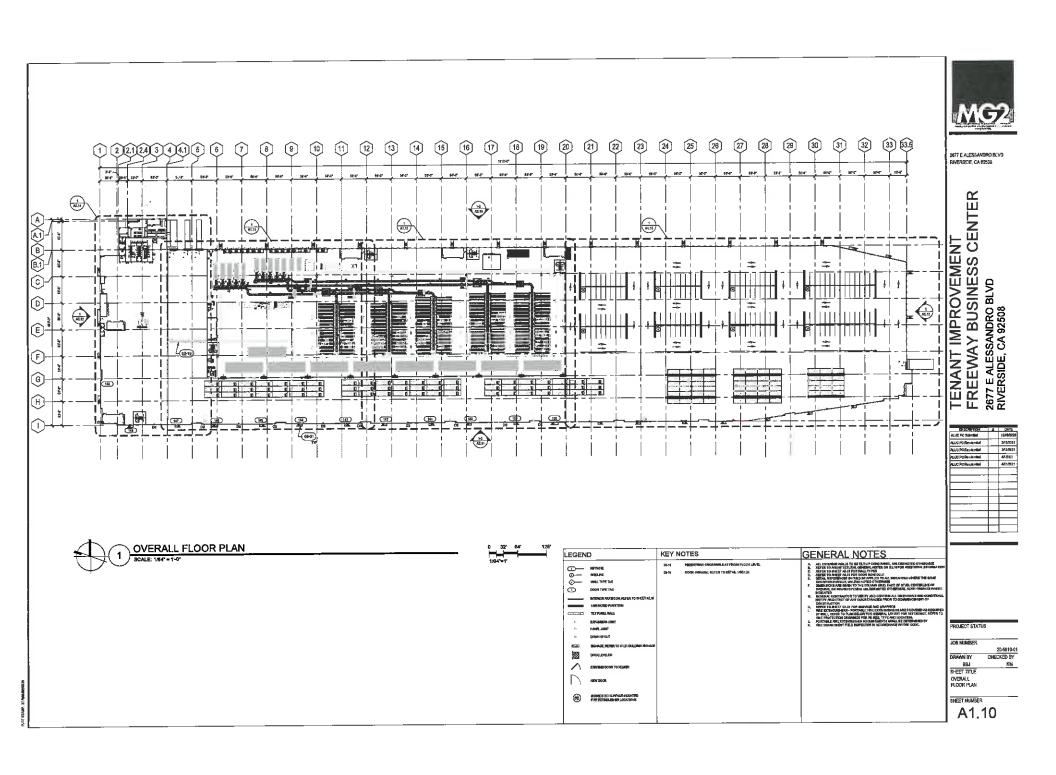
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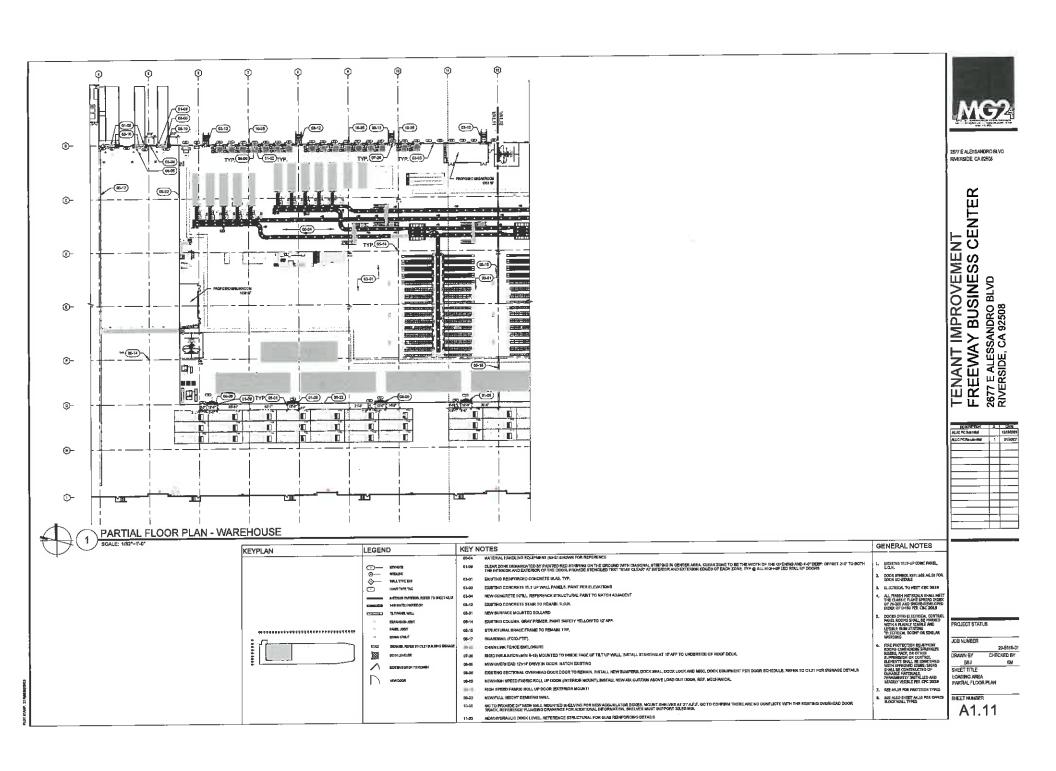
SINGLE ACRE DIAGRAM

20-5910-01

PM: KM

April 20, 2021







2677 E ALESSANDRO BLVD RIVERSIDE. CA 92608

TENANT IMPROVEMENT FREEWAY BUSINESS CENTER 2677 E ALESSANDRO BLVD RIVERSIDE, CA 92508

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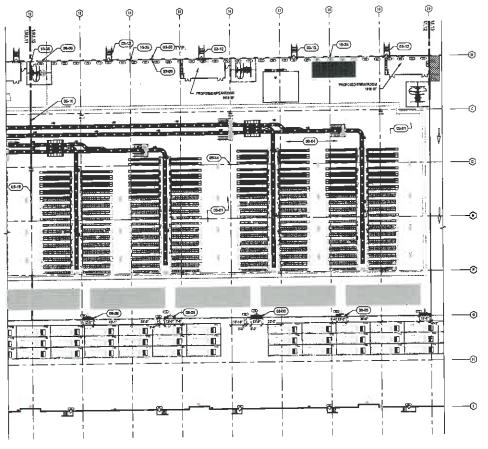
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SHEET TITLE LOADING AREA PARTIAL FLOOR PLAN

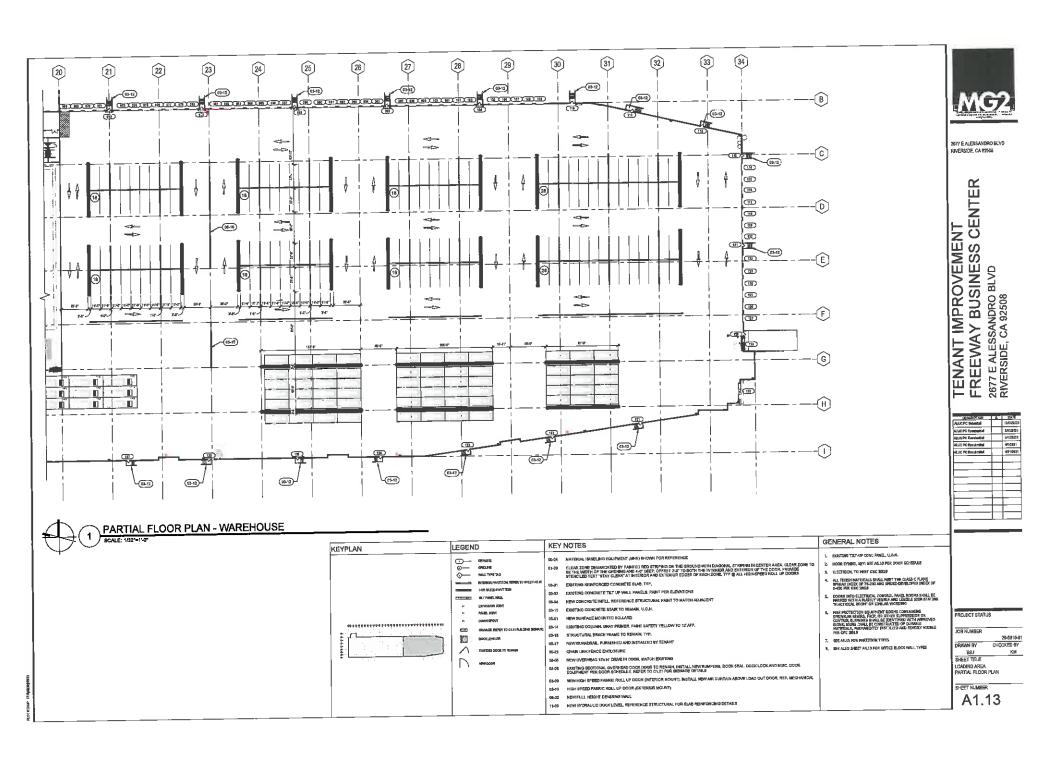
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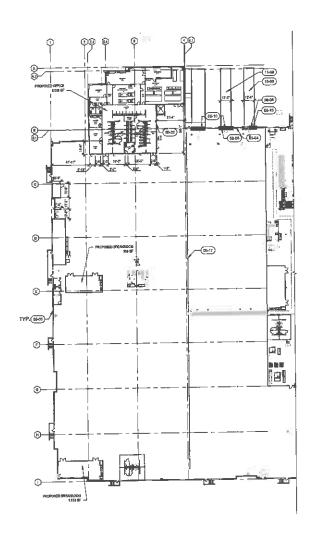
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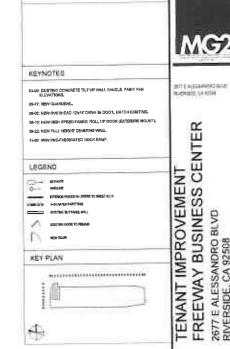


PARTIAL FLOOR PLAN - WAREHOUSE	
PARTIAL FLOOR PLAN - WAREHOUSE SCALE: 1/32*-01-0*	

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TENANT IMPROVEMENT FREEWAY BUSINESS CENTER 2677 E ALESSANDRO BLVD RIVERSIDE, CA 92508

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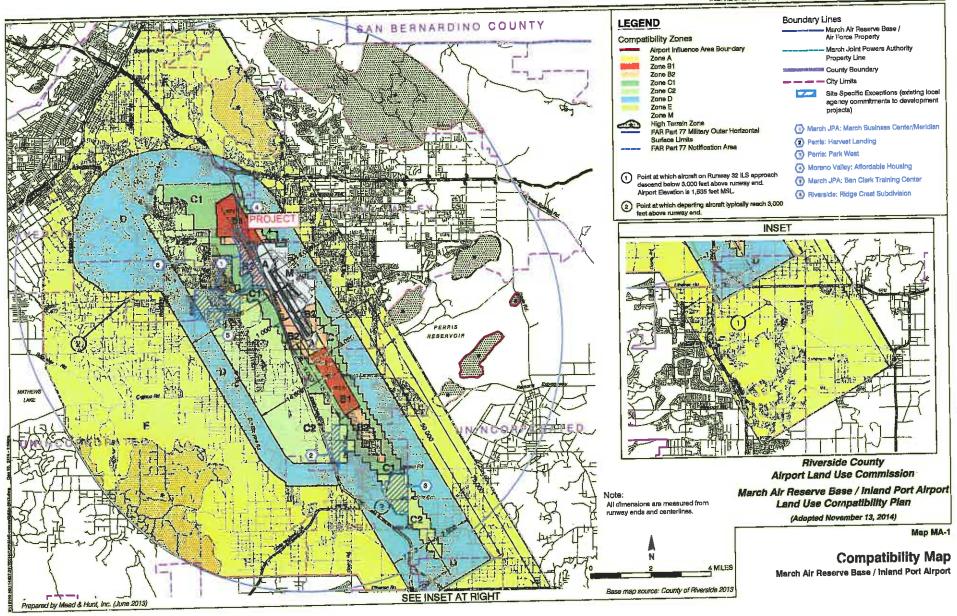
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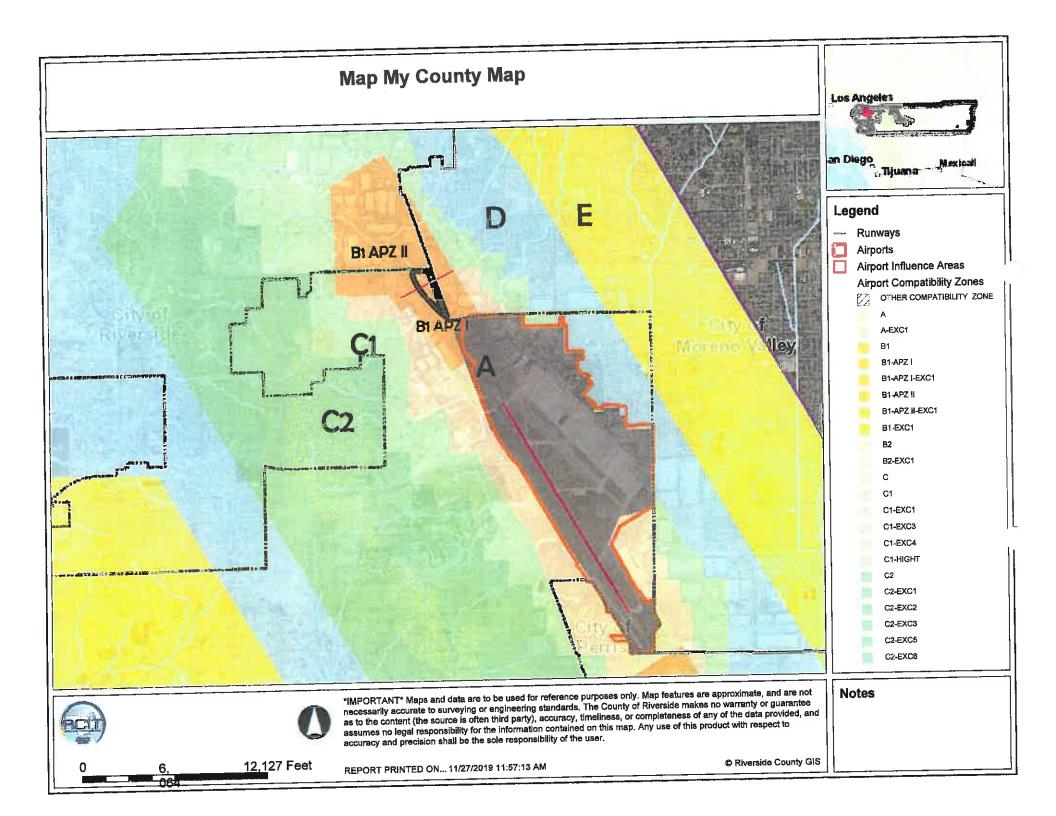
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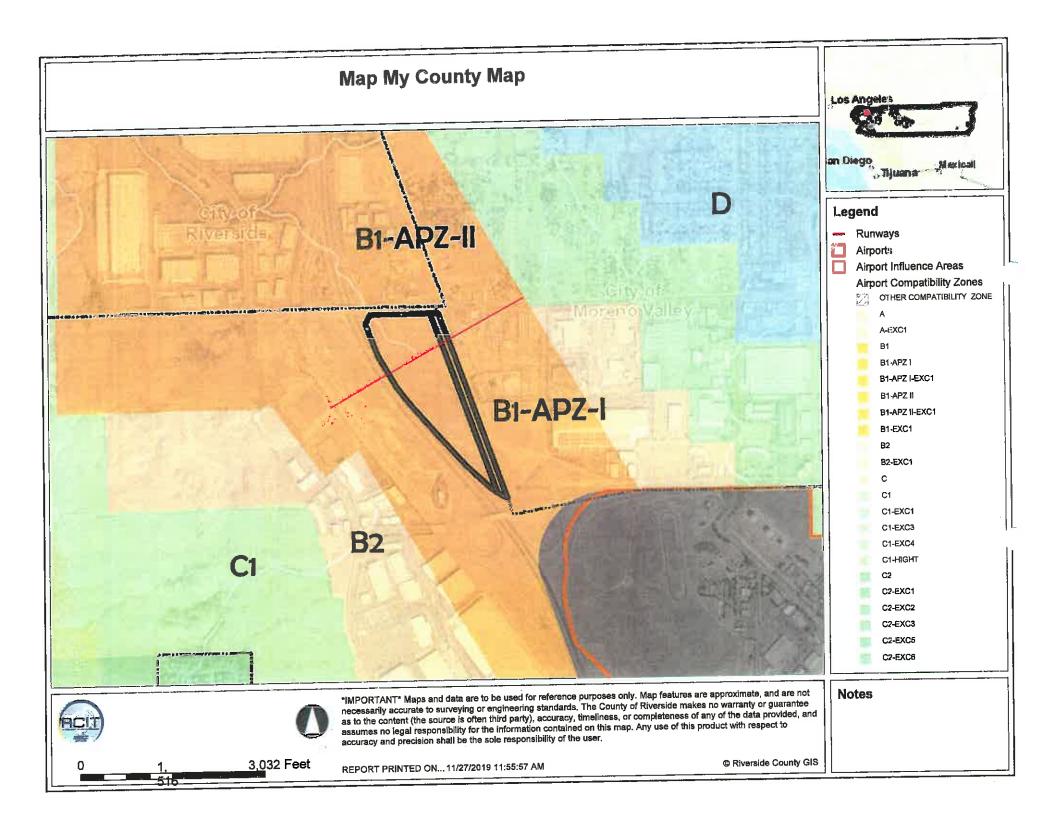
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PARTIAL FLOOR PLAN - OFFICE AREA













Legend

Blueline Streams

City Areas

World Street Map





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1, 3,032 Feet

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Legend

Blueline Streams

City Areas

World Street Map





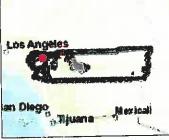
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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Riverside County GIS





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- Blueline Streams

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3, 6,064 Feet

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@ Riverside County GIS





Legend

Blueline Streams

City Areas

World Street Map





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3,032 Feet

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© Riverside County GIS



Rull, Paul

From:

Housman, Simon

Sent:

Tuesday, March 9, 2021 9:47 PM

To:

Rull, Paul

Subject:

Fwd: Freeway Business Center Amazon Delivery - ZAP1444MA20

Thank you. S.A. Housman

Begin forwarded message:

From: "MCCRAINE, RODNEY E Col USAF AFRC 452 MSG/CC" < rodney.mccraine.3@us.af.mil>

Date: March 9, 2021 at 21:25:31 PST

To: "Housman, Simon" <shousman@rivco.org>

Cc: "UNDERWOOD, HAMILTON B III Lt Col USAF AFRC 452 MSG/CD" <a href="https://www.ncentrologians.

AMW/JA" <samuel.welch.1@us.af.mil>

Subject: Freeway Business Center Amazon Delivery - ZAP1444MA20

Mr. Housman,

March ARB has reviewed the Airport Land Use Commission (ALUC) action; Freeway Business Center Amazon Delivery - ZAP1444MA20 (708k SF warehouse in APZs I and II, ALUC Zone B1). We find that the ALUC is recommending that a covenant be recorded on this property to restrict its use and occupancy to those established in Department of Defense Instruction 4165.57. However, because the enforcing government entity is March Joint Powers Authority (MJPA) we have questions over the enforceability of this covenant as well as two others that will be considered in April into the future and particularly if, and when, MJPA sunsets. We therefore ask for a continuance on the ALUC action until March ARB can determine what modifications, if any, should be made to the covenant to make it more enforceable.

Best Regards,

Rodney E. McCraine, Col, USAF Commander, 452 Mission Support Group March ARB, CA 92518 COMM 951-655-4221 (DSN 447)

Rull, Paul

From:

WATERS, DOUGLAS S GS-13 USAF AFRC 452 MSG/CE <douglas.waters.2@us.af.mil>

Sent:

Thursday, January 21, 2021 2:38 PM

To:

Rull, Paul

Cc:

Housman, Simon; MCCRAINE, RODNEY E Col USAF AFRC 452 MSG/CC; WELCH,

SAMUEL T Maj USAF AFRC 452 AMW/JA; MARTIN, DAVID R

Subject:

Fw: ZAP1444MA20 Freeway Business Center Amazon Delivery/Parcel Hub_Brian's Draft

Comments

Attachments:

Air Force MARB letter.pdf; March JPA_Amazon FBC_Rev Project Submittal.pdf; Amazon

FBC Warehouse_Proposed Site Plan.pdf; ALUC_Proposed Amazon FBC Site Location.pdf

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To meet your suspense for comments by COB Today, the following are March ARB comments as they concern the new proposed Amazon Parcel Distribution warehouse at JPA's Freeway Business Center.

Additional March ARB comments and a possible statement from leadership (Col Martin, Col McCraine, Col Morrison) could be given prior to or at the next public hearing on 11 February.

Doug Waters Chief Engineering Flight 452 MSG/CEC US Air Force Reserve Command 610 Meyer Dr., Bldg 2403 March ARB, CA 92518-2188

Douglas.waters.2@us.af.mil Office- 951-655-4852 Cell- 928-304-4852 DSN- 447-2197

- MARB Air Force Letter from 10 March 2015 (attached for reference): Since the revised proposed land use on this site (Parcel D3 East - Amazon distribution warehouse) is similar in scale to the previous proposed development (high-cube warehouse), MARB comments and concerns stated in attached AF letter remain valid as they concern:
 - A. Building height is not stated in plan review files received to date. Will require Airspace if greater than 35-foot height.
 - B. Water detention basins need to be oversized, covered, and/or underground to mitigate further rising groundwater table levels.
 - C. Stormwater management, drainage, and landscaping plans must adhere to BASH concerns.
 - D. Sound attenuation and hearing protection likely required for proposed office areas due to placement of building within the 65db AICUZ noise contour (CNEL).
 - E. As the proposed office space is contained entirely in APZ II, developer needs to clearly demonstrate compliance with AF and ALUC requirement of 50 persons per acre.

- F. Request ground elevation and building height to coincide with it. Obstruction Evaluation Service (OES) for potential finding of *Hazard to Air Navigation*, and to support MARB TERPS review.
- G. Require that construction materials be of non-reflective material, as well as down-lighting to mitigate glint/glare levels which could undermine safe air operations during evening hours.
- H. Proposed development shall not result in electronic and/or spectrum interference for aircraft communications or navigation capabilities.
- 2. Office Density Covenant: Both ALUC and March JPA mention a restrictive Covenant tied to allowable office densities as proposed in APZ II.
 - A. Does said Covenant need to be revised per the new proposed Amazon distribtuon facility?
 - B. MARB requests a copy of the previous covenant to assist with further review of the proposed project.
 - C. Need March JPA to confirm the latest proposed number of full time occupants at 518.
 - D. Discrepancies noted between the latest March JPA project transmittal (dated 21 Dec. 2020) for total office area of 25,000 SF (22,000 SF on ground floor and 3,000 SF mezzanine). Received site plan proposes a total of 18,715 SF.
 - E. Total allowable office SF per AF/AICUZ and ALUC standards is 18,000 SF. Does the Covenant attempt to justify an additional 7,000 SF of office space in APZ II? Requires clarification.
- 3. Traffic Impacts: MARB is concerned about substantially increased traffic levels on both Cactus Avenue and Heacock Drive, as it is assumed the proposed Amazon parcel distribution facility will draw heavily from existing and proposed air cargo facility expansion (e.g. D-1 Gateway Aviation Center) at the March Inland Port.

V

From: WATERS, DOUGLAS S GS-13 USAF AFRC 452 MSG/CE <douglas.waters.2@us.af.mil>

Sent: Monday, January 18, 2021 10:58 AM

To: Rull, Paul <PRull@RIVCO.ORG>; Pacino, Brian <Brian.Pacino@jacobs.com>

Subject: [EXTERNAL] RE: ZAP1444MA20 Freeway Business Center Amazon Delivery/Parcel Hub

Yes we will have comments to you by Thursday.

Douglas Waters Chief Engineering 951-655-4852

FY21 Year of March BCE

From: Rull, Paul < PRull@RIVCO.ORG > Sent: Monday, January 18, 2021 8:03 AM

To: Pacino, Brian < Brian. Pacino@iacobs.com >; WATERS, DOUGLAS S GS-13 USAF AFRC 452 MSG/CE

<douglas.waters.2@us.af.mil>

Subject: [Non-DoD Source] RE: ZAP1444MA20 Freeway Business Center Amazon Delivery/Parcel Hub

Importance: High

Good Morning,



DEPARTMENT OF THE AIR FORCE AIR FORCE RESERVE COMMAND

10 March 2015

MEMORANDUM FOR RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION ATTN: JOHN GUERIN
PRINCIPAL PLANNER
4080 LEMON STREET, 14TH FLOOR
RIVERSIDE, CA 92501

FROM: 452d Mission Support Group/Civil Engineers

Base Operating Support 610 Meyer Drive, Bldg. 2403 March ARB CA 92518-2166

SUBJECT: Riverside County Airport Land Use Commission (RCALUC) - ZAP1107MA14

- 1. The March Air Reserve Base (MARB) review of the proposal to establish Industrial zoning on 39.42 acres and to build a 709,083 square foot industrial warehouse building is provided with this memorandum.
- 2. The parcel also known as D3 East is located within the Accident Potential Zone I (APZ I) and Accident Potential Zone II (APZII). Any construction in APZ I is to consist of facilities that are no greater than single floor, airspace review is required for objects greater than 35ft. in height. Lot coverage is based on calculation in the Floor Area Ratios (FAR). Only a few types of facilities are compatible in this zone. There are restrictions on land uses and heights of natural objects and man-made objects in the vicinity of air installations that may obstruct the airspace, attract birds, cause electromagnetic or thermal interference, or produce dust, steam, smoke, or light emissions to provide for safety of flight and the public welfare.
- 3. The parcel is partially located within the Perris North sub-basin, the same groundwater sub-basin as MARB. The rising groundwater table at MARB is an ongoing concern and solutions are being sought by both MARB and the State of California. Given the concerns with the rising groundwater and the ability to properly drain the water detention basins within 48 hours, MARB is requesting the water detention basins be oversized enough to accept additional rock to address future concerns with groundwater or be underground and covered. Prior to issuance of formal approval, we want to see specific design of basins and if uncovered, calculations that show capacity if rock is added later.
- 4. A properly designed stormwater management system and landscaping must address Bird/Wildlife Aircraft Strike Hazard (BASH) concerns including proper detention/infiltration of stormwater runoff. The base will want to review details of the stormwater conveyance system and the landscaping plan when they become available. Given the proximity to the airfield, trees which will bear mast or grow to an adequate size for roosting should not be planted. Additional information on reducing BASH hazards can be found in AFPAM 91-212, Bird/Wildlife Aircraft Strike Hazard (BASH) Management Techniques, dated February 1, 2004. We request that March Joint Powers Authority evaluate the stormwater detention basin design to mitigate or eliminate any hazards, and jointly approve the design with MARB.
- 5. The property is impacted by aircraft noise with California Noise Equivalency Levels and a Day-Night Average A-Weighted Sound Level (DNL) of 65 decibels or more contributing to negatively impact

compatible and reasonable use of the property. Table 2, Land Use Compatibility in Noise Zones in Appendix 3 to Enclosure 3, Recommended Land Use Compatibility in Noise Zones of DoDI 4165.57, Air Installations Compatible Use Zones (AICUZ) states that noise events may be sufficient to periodically disrupt indoor activities. Employees and regularly received public may require protection using noise attenuation in the design and construction of the facility. Additional hearing protection for employees may be required by OSHA or other agencies as it relates to safety and health in a high noise level work environment.

- 6. While the proposed use may be consistent with the zoning and land use guidelines, MARB advises that the proposed project presents a concern being located so close to the Clear Zone (CZ) at the north end of the runway. Buildings in this area should not be used for high-density functions since the objective of the land use guidelines in and around APZ's is to restrict people-intensive use due to a greater risk of aircraft incident in these areas. In APZ I is restricted to 25 people per acre and 50 an acre in APZ II. Lot coverage is based on the FAR, and is calculated using standard parking generation rates for various land uses, vehicle occupancy rates, and desired density in APZ I and II. For APZ I, the formula is FAR = 25 people an acre/(Average Vehicle Occupancy x Average Parking Rate x (43560/1000)). The formula for APZ II is FAR = 50/(Average Vehicle Occupancy x Average Parking Rate x (43560/1000)).
- 7. The building height is a cause for concern. We request the latitude and longitude of the southeast and northeast corners and the ground elevation of the warehouse along this façade in order to confirm the building falls within established height restrictions. Consultation with the Federal Aviation Administration will be required and we will also need to provide a Terminal Instrument Procedures (TERPS) review.
- 8. To help eliminate any potential effects on aircraft operations at MARB, we ask that materials provided in construction be of a non-reflective material such as outside ductwork, windows and roofs by means such as painting or covering. In addition, none of the project improvements shall create:
 - Distracting lights which could be mistaken for airport lights
 - Sources of dust, steam, or smoke which may impair pilot visibility
 - Sources of electronic interference with aircraft communications or navigation
- 9. Thank you for the opportunity to again, review and comment on this proposed development. If you have questions please contact Ms. Denise Hauser at (951) 655-4862, or Sonia Pierce at (951) 655-2236.

Yamela M Hann PAMELA M. HANN

Base Civil Engineer

Scenario	Occupancy Ratio	Building Area	Total Occ.	Average Acre (ppl	Single Acre	Consistent w ALUC	Consistent w AICUZ
	High Cube Ware. 1/1,428	(sq.ft.) 699,583	630	per acre) APZ-I	(ppl) APZ-I	Ves	No.
[ALUC Standard Method] Utilizing the building code calculations methodology used in previously approved project ZAP1394MA19,	Office 1/200 Break Room 1/15	8,000 1,500	030	12	31		Yes, with the use of a
nd applying it to the proposed plan				APZ-II 21	APZ-II 130	No exceed single acre, break room + HCH warehouse	Covenant
[Hybrid Methodology] Building code calculating floor area usage, and	High Cube Ware. 1/1,428 Office 1/200 Break Room 1/15	258,000 8,000 1,500	518	APZ-I 10	APZ-I 52	Ves	No. Yes; with the use of a
parking code calculating the indoor van parking and stacking spaces. Parking code calculation is reduced to 1 person per space (vs standard 1.5 person per space) Doesn't account for floor area dead space.	Van parking space 1 Van loading space 1	70 126		APZ-II 17	APZ-II 100	Yes	Covenant
(previous staff report calculation) [Hybrid Methodology 1: Unidentified Floor Area] Building code calculating floor area usage, and	High Cube Ware. 1/1,428 Dead space as high cube Office 1/200	258,000 120,600 8,000	603	APZ-I 12	APZ-I 52	Yes	Wo. Yes, with the use of a
parking code calculating the indoor van parking and stacking spaces. Parking code calculation is reduced to L person per space (vs standard 1.5 person per space)	Break Room 1/15 Van parking space 1 Van loading space 1	1,500 70 126		APZ-II 19	APZ-II 120	No exceed single acre, break room + dead space HCH	Covenant
Building code requires all floor area to be identified. Therefore, the primary use of high-cube warehouse is applied to floor areas that are not identified.						warehouse	
[Building Code Strict] Building code does not have an occupancy calculation for high-cube warehouse, indoor van parking/stacking	Warehouse 1/500 Dead space as warehouse Office 1/200	699,583 8,000 1,500	1,523	APZ-1 35	APZ-I 87	No, exceed average acre	No. Yes, with the use of a
spaces. Therefore, a regular 'warehouse' ratio is applied per County Building & Safety.	Break Room 1/15			APZ-II 39	APZ-II 158	No. exceed single acre, break room + dead space	Covenant
Building code also requires all floor area to be identified. Therefore, the primary use of warehouse is applied to areas that are not identified.						warehouse	

Scenario	Occupancy Ratio	Building Area (sq.ft.)	Total Occ.	Average Acre (ppl per acre)	Single Acre (ppl)	Consistent w ALUC	Consistent w AICUZ
[Hybrid Methodology 2: All Floor Area Identified] Building code calculating floor area usage, and parking code calculating the indoor van parking and stacking spaces. Parking code calculation is reduced to 1 person per space (vs standard 1.5 person per space) Building code requires all floor area to be identified. The applicant has identified all previous unidentified floor areas on their floor plan	High Cube Ware. 1/1,428 Office 1/200 Break Room 1/15 Van parking space 1 Van loading space 1	307,000 8,000 5,400 164 126	905	APZ-I 13 APZ-II 40	APZ-II 377	No exceed single acre, break room 4 dead space HCH warehouse	No. Ves, with the use of a Covenant



Occupancy Survey for 2677 Alessandro Boulevard in Riverside County, CA

Site Surveyed: 9350 Rayo Avenue, South Gate, CA 90280

The Riverside County Airport Land Use Commission's (ALUC) Airport Land Use Plan permits the survey of similar uses as an acceptable method to determine concentrations of people for occupancy purposes. The proposed tenant has obtained a survey of a comparable use located at 9350 Rayo Avenue in South Gate, CA (the "comparable facility") and has attached such data. When Rayo's surveyed rate of 1 person per 70 SF is applied to the proposed 6,500 SF breakroom area of 2677 Alessandro Blvd., it would equate to a headcount of 93 people.

9350 Rayo Ave. is a logical analog facility for Alessandro: it is a parcel distribution station. The facility's operations are functionally equivalent with a similar capacity to the proposed quantities at 2677 Alessandro Blvd. At approximately 211,356 SF, the comparable facility is sized smaller than Alessandro; however, Alessandro's larger size relative to its operations is a direct function of efficiency as Rayo is significantly more efficient relative to its building square footage. The efficiency can be evidenced by the warehouse operational area within Alessandro, which is less than 37% of its building footprint.

Specifically, the data from 9350 Rayo Ave. was collected from the log of electronic security card-in points located adjacent to its 4,720 SF breakroom. The log includes "access granted" activity from February 1st – March 2nd, 2021 (30 days). The average count during the survey period averages to about 67 persons per daily period or approximately one person per 70 SF of breakroom area. The two buildings' operational capacity is expected to be roughly equivalent, and this is the most significant factor in their similarity for being comparable. When the one person per 70 SF surveyed occupancy rate is applied to the proposed 6,500 SF breakroom, it amounts to a 93-person headcount for occupancy calculation purposes. This is roughly half-way between an office use occupancy rate (1 person per 200SF) and the overly concentrated assembly use occupancy rate (1 person per 15 SF) that may be projected in the building code methodology and conservatively skews toward the latter. The surveyed data is a more realistic predictor for occupancy purposes, given that it represents real-life information as a proxy.

Given the foregoing, the data strongly suggests that applying the survey method to the proposed 6,500 SF Alessandro breakroom area would be a defensible and equitable method for occupancy purposes. Therefore, it is requested that an occupancy rate of 1 person 70 SF apply to the Alessandro breakroom.

1-Feb	97	9-Feb	65	17-Feb	67	25-Feb	6
2-Feb	68	10-Feb	67	18-Feb	69	26-Feb	7
3-Feb	73	11-Feb	76	19-Feb	66	27-Fe b	6
4-Feb	62	12-Feb	74	20-Feb	60	28-Feb	5
5-Feb	51	13-Feb	71	21-Feb	69	1-Mar	6
6-Feb	64	14-Feb	78	22-Feb	66	2-Mar	4
7-Feb	78	15-Feb	63	23-Feb	55		
8-Feb	74	16-Feb	60	24-Feb	73		

9350 Rayo Avenue, South Gate, CA 90280 Data

<u>System</u>	<u>Categories</u>	<u>Date</u>	Hour Starting	Hourly Headcount
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Debuted 7.4 Access Granted by Reader CUERY: START DATE: 2/2/2021 12:00:00 AMF, END DATE: 3/2/2021 11:50:09 PM, READERS: DAZY-1_202-PICK UP RM M, DAZY-1_204-READERDOM ENT CUERY: START DATE: 2/2/2021 12:00:00 AMF, END DATE: 3/2/2021 11:50:59 PM, READERS: DAZY-1_202-PICK UP RM M, DAZY-1_204-READERDOM ENT CUERY: START DATE: 2/2/2021 12:00:00 AMF, END DATE: 3/2/2021 11:50:59 PM, READERS: DAZY-1_202-PICK UP RM M, DAZY-1_204-READERDOM ENT CUERY: START DATE: 2/2/2021 12:00:00 AMF, END DATE: 3/2/2021 11:50:59 PM, READERS: DAZY-1_202-PICK UP RM M, DAZY-1_204-READERDOM ENT CUERY: START DATE: 2/2/2021 12:00:00 AMF, END DATE: 3/2/2021 11:50:59 PM, READERS: DAZY-1_202-PICK UP RM M, DAZY-1_204-READERDOM ENT CUERY: START DATE: 2/2/2021 12:00:00 AMF, END DATE: 3/2/2021 11:50:59 PM, READERS: DAZY-1_202-PICK UP RM M, DAZY-1_204-READERDOM ENT CUERY: START DATE: 2/2/2021 12:00:00 AMF, END DATE: 3/2/2021 11:50:59 PM, READERS: DAZY-1_202-PICK UP RM M, DAZY-1_204-READERDOM ENT CUERY: START DATE: 2/2/2021 12:00:00 AMF, END DATE: 3/2/2021 11:50:59 PM, READERDOM ENT CUERY: START DATE: 2/2/2021 12:00:00 AMF, END DATE: 3/2/2021 11:50:59 PM, READERS: DAZY-1_202-PICK UP RM M, DAZY-1_	Report Cate: 3/2/2021 1:5/:42PM Eastern Standard Time	Event Demis Event Details		436188698 343888757	40 41
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Originant 7.4 Access Granted by Reader QUERY; START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXY-1.2.05-PICK UP RIAIN, DAYY-1.2.05-PICK UP RIAIN, DAYY-1.2	Report Date: 3/2/2023, 1,579/2PM Eastern Standard Time Report Date: 9/2/2021, 1,57;42PM Eastern Standard Time	Event Details		436175458 241924115	46 47
De Guard 7.4 Access Granded by Rander QUERY: STANT DATE: 3/1/2021 12/2020 AM: END DATE: 3/1/2021 11/2020 PM: BEATING: DATE-1,200-PICK UP RM IN, DAY-1,210-BREAKROOM ENT	Report Date: 3/2/2021 1:57:12PM Eastern Standard Time	Event Details		436192950 289219546	48 49
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On Guard 7.4 Access Granted by Reeder QUERY: START DATE: 4/1/AXX1123000 AM; END DATE: 3/1/2021 11:59:39 PM; READERS: DAXY-12:03 PXC UP REVIX; DAXY-12:04-9808 PM; READERS: DAXY-12:05 PXC UP REVIX; DAXY-12:04-9808 PM; READERS: DAXY-12:05 PXC UP REVIX; DAXY-12:04-9808 PM; READERS: DAXY-12:05 PXC UP REVIX; DAXY-12:04-9808 PXC UP PXC U	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/2/2021. Access Granted 2/2/2021. Access Granted	212763722 341910854	58
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OrrGuard 7.4 Access Granted by Breider QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; REAGERS: DAVY-1,2:08-PICK UP Rid PIC DATE: 1/2/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; REAGERS: DAVY-1,2:08-PICK UP Rid PIC DATE: 1/2/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; REAGERS: DAVY-1,2:08-PICK UP Rid PIC DATE: 1/2/2021 12:00:00 AM; END DATE: 1/2/2021 11:59:59 PM; REAGERS: DAVY-1,2:08-PICK UP RID PIC DATE: 1/2/2021 12:00:00 AM; END DATE: 1/2/2021 11:59:59 PM; REAGERS: DAVY-1,2:08-PICK UP RID PIC DATE: 1/2/2021 11:59:59 PM; REAGERS: DAVY-1,2:08-PICK UP RID PIC DATE: 1/2/2021 11:59:59 PM; REAGERS: DAVY-1,2:08-PICK UP RID PIC DATE: 1/2/2021 11:59:59 PM; REAGERS: DAVY-1,2:08-PICK UP RID PIC DATE: 1/2/2021 11:59:59 PM; REAGERS: DAVY-1,2:08-PICK UP RID PIC DATE: 1/2/2021 11:59:59 PM; REAGERS: DAVY-1,2:08-PICK UP RID PIC DATE: 1/2/2021 11:59:59 PM; REAGERS: DAVY-1,2:08-PICK UP RID PIC DATE: 1/2/2021 11:59:59 PM; REAGERS: DAVY-1,2:08-PICK UP RID PIC DATE: 1/2/2021 11:59:59 PM; REAGERS: DAVY-1,2:08-PIC DATE: 1/2/2021 11:59 PM; REAGERS:	Report Cate: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Ontalls	2/2/2021 Assess Granted	343881027	61
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O MENY, START DATE: 2/1/2021 12:00:00 AM: END DATE: 2/2/2021 11:59:59 PM; READERS: DAX7-1.2:09-PKX UP RM IN, DAX7-1.2:09-P	Report Cate: 2/2/2021 1:57;42PM Eastern Standard Time Report Date: 2/2/2021 1:57;42PM Eastern Standard Time	Event Details Event Datails	2/2/2021 Access Smitted 1/3/1021 Access Sranted	341926735	1
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On Bland 7.4 Access Granted by Reader QUERY: STAR DATE: 2/1/2011 2/0000 AM: FIND DATE: 3/2/2011 115/555 PW; IRRADIES: DAY-12.03-FIND DATE: 3/2/2011 115/555 PW; IRRADIES: DAY-12.03-FIND DATE: 3/2/2011 115/555 PW;	Report Date: 1/2/2021 1:57:42PM Eastern Standard Time	Event Datells	2/3/2021 Access Granted 2/2/2021 Access Granted	341897282 341922904	14
OnGuard 7.6 Access Granted by Randar QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 8/2/2021 11:59:59 PM; READERS: DADY-12:08-PRCK UP RM 98, DAY-12:08-PRCK UP RM 98, D	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Octobs	2/3/2021 Access Granted	2415925@	15
CHEST STATE DATE: 2017/00117-0000 AM: FIND DATE: 2/27/0011159:55 PM: READERS: DAX7-1-2-08-PICK UP RM IN, DAX7-1-2-04-BREAKROOM ENT	Report Date: 1/2/2021 157:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/3/2021 Access Granted 2/3/2021 Access Granted	341890253 436193080	16 17
On Guard 7.4 Access Stanford by Reader QUERY: START DATE: 2/1/2011 12:00:00 AM; EMD DATE: 3/2/2011 13:50:09 PM; SEARCHS: DATE-LLD-FILE or REVIEW AND ACCESS TO ACCESS	Report Date: 3/2/2021 1:57:62PM Eastern Standard Time	Event Datella Front Outsile	2/3/2021. Access Granted	341426627 410439833	18 19
On Guard 7.4 Appear Granted by Reader QUERT: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/3/2021 11:5R:39 PM; READERS; DAXT-1.2/07-PXX UP RM NI, DAXT-1.2:01-SREAR DOMESTING	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/3/2021 Azzess Granted 2/3/2021 Azzess Granted	241913079	20
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On Guerti 7.4 Accesse Genthed by Resider CUERTY: START DATE: 2/1/2011 13:00:00 ANY; END CATE: 3/2/2021 11:39:59 PM; READES: 00X7-12.03-PICK UP BM HI, DATE-12.03-READEM CONTROL 11:00 PM HI CONTROL 12.03-PICK UP BM HI CONTROL 12	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details	2/5/2021 Access Granted	341974116	23
On Grant J.A. Access Grant and by Reader QUERY: START DATE: 2/1/2021 12:00:001 AM; END DATE: 3/2/2021 13:59:59 PM; READERS: DAYF-12-02-PICK OF RM Nr., DADY-12-02-PICK OF RM Nr., DADY-	Report Date: 3/2/2021 1:57:42PM Eartern Standard Time Report Clate: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/3/2021 Access Grented 2/3/2021 Access Grented	341924094 410432665	24 25
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OriGulard 7.4 Access Statuted by Render QUERY: START OATE: 2/1/2021 12:00:00 AM; SPD DATE: 2/1/2021 11:59:59 PM; READERS: DAGG-12:03-PICK UP IM N, DAGG-1-12:04-PICK UP IM N, DAGG-1-12	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datails	2/3/2021 Access Grented	341919622	28
ONE OF THE PART PART PART PART PART PART PART PART	Report Date: 5/2/2021 5:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/3/2021 Access Granted 2/3/2021 Access Granted	435174401 435174665	25 30
OmSuind 7.4 Access Granted by Render OUERY: START DATE: 24/2021 12:0000 AM; END DATE: 3/2/2021 12:000 PM; READERS; DADY-1_2.00-ED MM (I) DAY-1_2.00-REARRS; DADY-1_2.00-ED MM (I) DAY-1_2.00-REARRS; DADY-1_2.00-ED MM (II) DAY-1_2.00-REARRS; DADY-1_2.00-ED MM (II) DAY-1_2.00-REARRS; DADY-1_2.00-ED MM (II) DAY-1_2.00-REARRS; DADY-1_2.00-ED MM (III) DAY-1_2.00-REARRS; DADY-1_2.00-ED MM (III) DAY-1_2.00-REARRS; DAY-1_2.00-ED MM (III) DAY-1_2.00-REARRS; DAY-1_2.00-ED MM (III) DAY-1_2.00-REARRS; DAY-1_2.00-REA	Report Date: 3/2/2023, 1:57:42FM Eastern Standard Time	Event Details	2/3/2021 Access Grented	341913565	91 32
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OnGoord Z.A. Access Granked by Meeter CUERTY: SHARE DUTE: 2/1/2011 12:00:000 AMI; BID DATE: 2/1/2011 11:00:05 PM; READERS: DAT/-1.206-FICK UP RAY IN, OACT-1.2:04-BEARRICOM ENT ONGORD Z.A. Access Granked by Reader CUERTY: STATE DATE: 2/1/2011 12:00:000 AMI; BID DATE: 3/1/2011 11:00:05 PM; READERS: DAT/-1.2:04-FICK UP RAY IN, DAT/-1.2:04-BEARRICOM ENT ONGORD Z.A. Access Granked by Reader CUERTY: STATE DATE: 2/1/2011 12:00:000 AMI; BID DATE: 3/1/2011 11:00:05 PM; READERS: DAT/-1.2:04-FICK UP RAY IN, DAT/-1.2:04-BEARRICOM ENT	Report Date: 3/2/2021 1:57:42FM Eastern Standard Time	Event Datalis Event Datalis	2/5/2021 Access Grented 2/5/2021 Access Grented	341922970 341890520	34 35
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CHERRY CTART DATE: 2/1/2021 12:00:00 AM: END DATE: 1/2/2021 11:59:59 PM: REIOTRS: DAX7-1.2.03-PICE UP RM: IN, UAR7-1.2.03-BREMANDUM ENT	Report Date: 3/1/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/3/2021 Access Granted 2/3/2021 Access Granted	341888757 341924513	27 28
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THE PARTY STATE DATE: 7/1/2021 12:00:00 AM: END DATE: 3/2/2021 21:50:50 PM: READERS; DAG-1.2.03-PICK UP RM. IV, DAG-1.2.03-00:24K0.03M END	Report Cate: 2/2/2021 1:57:42PM Eastern Stendard Time Report Date: 2/2/2021 1:57:42PM Eastern Stendard Time	Event Outsits Event Details	2/3/2021 Access Granted 2/3/2021 Access Granted	341922970 426188698	56 54
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On Guard 7.4 Access Granted by Sendar QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:39 PM; SEADERS: QAX7-12.20-SEADERS-ACK/UP RM N, DAX7-12.20-SEADERS-ACK/UP RM N, DAX7-1	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Oate: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/3/2021 Access Granted 2/3/2021 Access Granted	341922970 341890498	56 57
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OnGuard 7.4 Access Branned by Bourder QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:58:59 PM; READERS: DAX7-1.2-CE-PICK UP RM IN, DA07-1.2-CO-PICK UP RM IN, DA07-1.2-	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/3/2021. Access Granted 3/3/2021. Access Granted	341911449 232763722	86 67
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OUTSTAND AND A COUNTY STANT DATE: 3/1/2021 12-00:00 AM: FND DATE: 3/2/2021 11:59:59 PM; READERS; DAX7-1.2.00-PICK UP RIM IN, DAX7-1.2.01-RESERVATION LITT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 3:57:42PM Eastern Standard Time	Event Detail		341906GB 289219346	71
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OnGuard 7.4 Access Greened by Busider CLUERY: START DATE: 2/1/2021 12:0000 AM; END DATE: 3/1/2021 11:0000 PM; TEADERS: DAST-1.2.03-PICX UP RMIN, DAYS-1.2 UP	Report Date: 3/2/2021, 1:57;42PM Eastern Standard Time Report Date: 3/3/2021, 1:57;42PM Eastern Standard Time			341913387 341890101	2
Consider A Access General by Read or Outs, 1947 (1992) 120000 AN; DID DATE: \$7/2021 110000 FM; DID DATE	Report Date: 3/2/2021 1:57:42914 Eastern Standard Time	Event Detail	2/4/2021 Access Granted	341913189 410831727	1 4
OriGuard 7.4 Access Granted by Reader QUERY: START DATE: \$1/\$203,12:0000 ANE, 98/\$2/2031 13:3559 PM; READERS: 0007-1.2.05-PMC UP RMIN, DATY-1.2-04-READERS AND AND ANT START DATE: \$1/\$203,12:0000 ANE, 98/\$2/2031 13:959 PM; READERS: 0007-1.2.05-PMC UP RMIN, DATY-1.2-04-READERS AND ANT START DATE: \$1/\$203,12:0000 ANE, 98/\$2/2031 13:959 PM; READERS: 0007-1.2.05-PMC UP RMIN, DATY-1.2-04-READERS AND ANT START DATE: \$1/\$203,12:0000 ANE, 98/\$2/2031 13:959 PM; READERS: 0007-1.2.05-PMC UP RMIN, DATY-1.2-04-READERS AND ANT START DATE: \$1/\$203,12:0000 ANE, 98/\$2/2031 13:959 PM; READERS: 0007-1.2.05-PMC UP RMIN, DATY-1.2-04-READERS AND ANT START DATE: \$1/\$203,12:0000 ANE, 98/\$2/2031 13:959 PM; READERS: 0007-1.2.05-PMC UP RMIN, DATY-1.2-04-READERS AND ANT START DATE: \$1/\$203,12:0000 ANE, 98/\$2/2031 13:959 PM; READERS: 0007-1.2.05-PMC UP RMIN, DATY-1.2-04-READERS AND ANT START DATE: \$1/\$203,12:0000 ANE, 98/\$2/2031 13:959 PM; READERS: 0007-1.2.05-PMC UP RMIN, DATY-1.2-04-READERS AND ANT START DATE: \$1/\$203,12:0000 AND	Report Date: 3/2/2021. 1:57:42PM Sestem Standard Time Report Date: 3/2/2021. 1:57:42PM Eastern Standard Time	Event Dates	2/4/2021 Access Sminted	341890238	5
CHERY, START DATE: 1/1/2/02 12:00:00 AM: PND DATE: 1/2/2/02 11:59:59 PM; READERS; DAX7-12:03-90X UP AM PI, DAX7-12:03-90X AM PI, DAX	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detail Event Cetail	b 2/4/2021 Access Grented	341892414 34197554E	6 7
Cinduard 7.A Access Branded by Render CULDIV: STANT DATE: 2/1/2021 12:0000 ANJ: 89/2/2021 11:59:59 PM; RENDERS; CM07-1.202-PLC; UP RWIN, DATE-1.207-98:000000 RM; PM CONTES 3/1/2021 13:59:59 PM; RENDERS; CM07-1.203-PLC; UP RWIN, DATE-1.207-98:00000000 RM; PM CONTES 3/1/2021 13:59:59 PM; RENDERS; CM07-1.203-PLC; UP RWIN, DATE-1.207-98:0000000000 RM; PM CONTES 3/1/2021 13:59:59 PM; RENDERS; CM07-1.203-PLC; UP RWIN, DATE-1.203-PLC; UP RWIN, DATE-	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datel	is 2/4/2021 Accuss Granted	341890988 341924160	8 9
OriGuard 7.4 Access Grapted by Reader CUERT: START DATE: 2/1/2021 12:00:00 AM; END OATE: 3/7/2021 13:5935 PM; READER: DATE: 2/1/2021 PM; READER: 2/1/2021 PM; REA	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detail	is 2/4/2021 Access Stremed	341912991	10
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Ortiguard 7.4 Access Granted by Reader: OURRY: START DATE: 3/1/2/03.1129.000 AM: END DATE: 3/2/2/03.1129.99 PM; READERS: DATE: 1/2/2/03.1129.99 PM; READERS: DATE: 1/2/2/2/2/2/2/2/2/2/2/2/2/2/2/2/2/2/2/2	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Tim	Event Deba	Es 2/4/2021 Access Granted	341925372 341924336	16 17
Children's 7.4 Access Granted by Reader CALEXY: STATI DATE: 2/1/2011 12:0000 ANY: 6400 DATE: 3/2/2011 13:0000 ANY: 6840 BS: DOXY-1-2:04-9000 BS: DOXY-1-2:0	Report Date: 3/2/2021 1:57:42PM Eastern Standard Tim Report Date: 3/2/2021 1:57:42PM Eastern Standard Tim	: Event Date	in 2/4/2021 Access Granted	496175453	18
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CinGuard 7.4 Access Granted by Reside: QUERY, START DATE: 2/1/221.12.0000 AN; BED DATE: 3/2/201115959 PM; READERS: DBX7-1.2.03-PMC UP WHITE, DAX7-1.2.04-personal own entities and start of the control o	Report Date: 3/2/2021 1:57:42PM Eastern Standard Tire	Event Dete	ills 1/4/2021 Access Granted	341913365 438193060	21 22
DoSuard 7.4 Acress Greated by Resider OURS: STATE DATE: 1/1/2011 12:00000 ANY: END DATE: 2/1/2011 13:59159 PM; REDUCES: DADY-1.2,103-PICU PR RAIN, DUKT-1.2,103-PICUPE RAIN, PICUPE RAIN,	Report Date: 3/2/2021 3:57:42PM Eastern Standard Tim Report Date: 3/2/2021 1:57:42PM Eastern Standard Tim	Brent Deb	lle 2/4/2021. Access Smrttes	410433866	23
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On Suard 7.4 Access Granted by Reeder On Buard 7.4 Access Granted by Reeder	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:59:59 PM; READESS: DAXT-1.2.09-PICK UP RM IN, DAXT-1.2.04-BREAKROOM ENT QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/1/2021 11:59:59 PM; READERS: DAXT-1.2.09-PICK UP RM IN, DAXT-1.2.01-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/4/2021 Access Granted	341925350	30 31
OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Granted by Reader	CUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2021 11:59:55 PM; READERS: DAXT-1,2,03-PICK UP RAMIN, DAXT-1,2.04-BREAKROOM ENT QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:56 PM; READERS: DAXT-1,2.03-PICK UP RAMIN, DAXT-1,2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Datails	2/4/2021 Access Granted 2/4/2021 Access Granted	34188757 341924226	32
On Quarti 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; BND DATE: 2/2/2021 11:59:59 PM; READERS: DAYT-1.2.09-F/CK UP RM IN, DAX7-1.2.04-BREADROOM ENT	Report Date: 8/2/2021 1:57:42PM Eastern Standard Time Report Date: 8/2/2021 1:57:42PM Eastern Standard Time	Event Outsils Event Details	2/4/2021 Access Granted 2/4/2021 Access Granted	410430031 266315411	15 34
On Stient 7.4 Access Granted by Reader On Buard 7.4 Access Granted by Reader	QUERY: START DATE: 1/1/2011 12:00:00 AM; END DATE: 2/2/2021 11:59:59 PM; READING: DAYY-12:08-PMX UP RM IN, DAYY-12:04-BREADROOM ENT CUERY: START DATE: 1/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READING: DAYY-12:08-PMX UP RM IN, DAYY-12:04-BREADROOM ENT	Report Oate: 3/2/2021, 1:57942PM Eastern Standard Time	Event Details	2/4/2021 Access Grunted	341924958	35
GnGuard 7.4 Access Granted by Render OnGuard 7.4 Access Brented by Render	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 9/2/2021 11:59:59 PM; READERS: DADY-1.2:05-PCK UP RM IN, DAXY-1.2:04-READERSCOM ENT QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 9/2/2021 11:59:59 PM; READERS: DADY-1.2:05-PCK UP RM IN, DAXY-1.2:04-REAXROOM ENT	Report Date: 1/2/2021, 1:57:42PM Restam Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details	2/4/2021 Access Granted 2/4/2021 Access Granted	23583864E 436177617	36 37
OnGuerd 7.4 Appeas Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11:59:59 PM; READERS: DADT-1.2:09-PiCK LIP RM IN, DAX7-1.2:04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Ensieth Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/4/2021 Access Stanted 2/4/2021 Access Stanted	341925284 436188498	3R 39
On Guard 7.A Access Granted by Reader On Guard 7.A Access Granted by Reader	CURRY: START DATE: 2/1/2021 12:0000 AM; BND DATE: 3/2/2021 11:99:SS PM; READERS: DAXY-12,03-PICK UP RM IN, DAXY-12,04-BREADROOM ENT DUBRY: START DATE: 2/1/2021 12:00:00 AM; BND DATE: 3/2/2021 11:99:S0 PM; READERS: DAXY-12,03-PICK UP RM IN, DAXY-12,04-BREADROOM ENT	meport Jate: 3/2/2021 #57:42-m Enstern Standard Time	Evens Details	2/4/2021 Access Stanled	436183720	40
On Street 7.4 Access Granted by Reader On Street 7.4 Access Scentad by Reader	QUERY: START DATE: 2/1/2031 12:00:00 AM; END DATE: 3/2/2031 11:59:59 PM; READERS: DUXY-1.2:03-PIOCUP RM IX, DAXY-1.2:04-BREADERO MI ENT QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2011 11:59:59 PM; READERS: DAXY-1.2:03-PIOCUP RM IX, DAXY-1.2:04-BREADROOM ENT	Report Date: 3/2/2021 1:57:42PM Sestem Standard Time Report Date: 3/2/2021 1:57:42PM Sestem Standard Time	Event Details Event Details	2/4/2021 Access Granted 2/4/2021 Access Granted	289218546 261669595	41 42
OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2:05-PKX UP RM IN, DAX7-1.2:06-BIEAGROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/4/2021 Access Granted 2/4/2021 Access Granted	172905979 436175458	43 44
On Guard 7.4 Access Granted by Reader On Guard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:0000 AM; SND DATE: 1/2/2021 13:9:59 PM; READINS: DADY-1.2:04-PICK UP RM IN, DAX7-1.2:04-REAKROOM ENT QUERY: START DATE: 2/1/2021 12:00:00 AM; SND DATE: 3/2/2021 12:9:59 PM; READINS: DADY-1.2:04-PICK UP RM IN, DAX7-1.2:04-REEAKROOM ENT	Report Date: 3/2/2021 1:57:43PM Festim Standard Time	Event Details	2/4/2021. Access Granted	341891952	45
On Guard 7,4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:39:59 PM; READERS: DAT-1_2.03-FDCK UP RM IN, DAX7-1_2.04-BREADERS DAY-1_2.03-FDCK UP RM IN, DAX7-1_2.04-BREADERS DAY-1_2.04-BREADERS DAY-1_2	Report Date: 1/2/2021 1:57:42PM Emisson Standard Time Report Date: 9/2/2021, 1:57:43PM Emisson Standard Time	Event Details Event Details	2/4/2021 Access Grented 2/4/2021 Access Grented	341922970 410432879	48 47
On Buard 7.4 Access Granted by Reader On Guard 7.4 Access Granted by Reader	QUERT: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1, 2.03-PICK UP RM IN, DAX7-1, 2.04-BIEARRIGOM ENT	Report Date: 2/2/2011 1:57:42PM Eastern Standard Time Report Oate: 2/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/4/3071 Access Granted 2/4/3071 Access Granted	251909140 341924228	46
OnGuard 7.4 Access Granted by Reader DisSuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 9/2/2021 11:59:59 PM; READERS: DAV7-1.2.03-PKK UP RM IN; DAV7-1.2.03-BREAKROOM ENT QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 9/2/2021 11:59:59 PM; READERS: DAV7-1.2.03-PKK UP RM IN; DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datalis	2/4/2021 Access Granted	3416904FB	50
Dn Buard 7.4 Access Granted by Reader	QUERY: START DATE: 2/4/2021 12:00:00 AM; END DATE: 3/2/2021 11:50:59 PM; READERS: DATO-12:09-PICK UP RIAI IN, DAX7-12:04-BREAKROOM ENT QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:50:59 PM; READERS: DATO-12:09-PICK UP RAI IN, DAX7-12:04-BREAKROOM ENT	Report Date: 3/2/2021, 157:42PM Eistern Standard Time Report Date: 3/2/2021, 157:42PM Eastern Standard Time	Event Datails Event Datails	2/4/2021 Acress Branted 2/4/2021 Acress Branted	124691775 251334739	51 52
On Guard 7.4 Access Granted by Reader On Guard 7.4 Access Granted by Reader	DUSBAY: START DATE: 2/1/2021 12:00:00 AM: BNO DATE: 3/2/2021 11:59:99 PM; READBYS: DAIG-1.2:08-PICK UP RM IN, DAX7-1.2:04-BREAKROOM ENT	Report Date: 3/2/2021 157:42FM Eastern Standard Time	Event Demils Event Demils	2/4/2021 Access Granted 2/4/2021 Access Granted	341919864 289218731	53 54
On/Guard 7.4 Access Granted by Reader On/Guard 7.4 Access Branted by Reader	QUERY: START DAYE: 2/1/2021 12:00:00 AM; BND DATE: 3/2/2021 11:59:59 PM; READERS: DAXT-12:00-PICK UP RM/IN, DAEZ-12:04-BREADROOM ENT QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXT-12:03-PICK UP RM/IN, DAXT-12:04-BREADROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/4/2021 Access Granted	232760722	55
OnGamed 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2011 12:00:00 AM; END DATE: 3/2/2011 11:59:59 PM; READERS: DAX7-1.2.03-PICK UF RM IN; DAX7-1.2.04-BREARROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datells Event Datells	2/4/2021 Access Granted 2/4/2021 Access Granted	251307818 2905£3553	56 57
On Guard 7.4 Access Granted by Reader On Guard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00RTAM; END DATE: 3/2/2021 11:5959 PM; READERS: DAKY-1.2:03-PKC UP SM IN. DAXY-1.2:04-9KEAKROOM ENT QUERY: START DATE: 1/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:39:39 PM; READERS: DAKY-1.2:03-PKC UP RM IN; DAKY 1.2:04 BREADEROOM ENT	Report Pate: 3/3/3031 1:57:42PM Eastern Standard Time	Event Datalls Event Details	2/4/2021 Access Granted 2/4/2021 Access Granted	261672745 341911449	58 56
On@used 7.4 Access Seented by Reader OnGuard 7.4 Access Grented by Reader	QUERY: START DAYE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAYG-1_2:08-PICK UP RM IN, DAX7-1_2:08-BREAKROOM ENT QUERY: SCART DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXG-1_2:08-PICK UP RM IN, DAXG-1_2:08-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 9/2/2021 1:57:42PM Eastern Standard Time	Event Detells	2/4/2021 Access Grephed	218611830	60
OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2:09-PICK UP RM IN, DAX7-1.2:09-BREARROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/4/2021 Access Granted 2/4/2021 Access Branted	341889908 289219546	61 62
OrnGuard 7.4 Access Granted by Reader OrnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2011 12:00:00 AM; END DATE: 8/2/2011 11:58:59 PM; READERS: DAX7-1.2:09-PICK UP RM IN, DAX7-1.2:09-BREADROOM ENT QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2:09-PICK UP RM IN, DAX7-1.2:09-BREADROOM ENT	Report Date: 3/2/2021 1:57:42PM Factors Standard Time	Event Details	2/5/2021 Access Granted	341913387	1
OnGuard 7.4 Access Granted by Reader	DUBRY: START DATE: 1/1/2021 12:0000 AM; SHD DATE: 1/2/2021 11:50:39 PM; READBUS: DAX7-1.2:03-PKX UP RM IN, DAX7-1.2:00-BREAKROOM ENT QUBRY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:39 PM; READBIS: DAX7-1.2:03-PKX UP RM IN, DAX7-1.2:00-BREAKROOM ENT	Report Date: 9/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Datails	2/5/2021 Access Granted 2/5/2021 Access Granted	341890388 841925272	3
On Guard 7.4 Access Stanted by Reader On Guard 7.4 Access Branted by Reader	QUERY: 57ART DATE: 2/1/2021 12:00:00 AM) END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2.03-PICK UP RM (N. DAX7-1.2.04-BREAKROOM ENT	Report Date: 9/2/2021 1:57:42PM Eastern Standard Time Report Date: 2/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/5/2021 Access Granted 2/5/2021 Access Granted	941890233 341890454	4 5
On Guard 7.4 Access Granted by Reader On Guard 7.4 Access Granted by Reader	QUERY; START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/1/2021 11:59:59 PM; READERS: DROF-1.2,08-PDX UP RM IN, DACF-1.2,08-READROOM ENT QUERY; START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/1/2021 11:59:59 PM; READERS: DROF-1.2,08-RDX UP RM IN, DACF-1.2,08-RDX DATE: DROF-1.2,08-RDX DATE: 2/1/2021 11:59:59 PM; READERS: DROF-1.2,08-RDX UP RM IN, DACF-1.2,08-RDX DATE: DROF-1.2,08-RDX DATE: DROF-1.2,0	Report Date: 3/2/2021, 1:57:42PM Sastem Standard Time	Event Details	2/5/2021 Access Granted	341892082	
On Buard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:99:59 PM; NEADERS: DAZY-1.2.09-PICK UP RM IN, DAXY-1.2.04-BREAKROOM ENT QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXY-1.2.00-PICK UP RM IN, DAXY-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:43PM Sectom Standard Time Report Date: 3/2/2021 1:57:43PM Eastern Standard Time	Event Details	2/5/2021 Access Greeted 2/5/2021 Access Greeted	341913189 438174708	7
On@uerd 7.4 Access Granted by Reades On@uerd 7.4 Access Branted by Reades	QUERY: START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 13:59:59 PM; READERS: DAX7-12.03-PKK UP RM:N, DAX7-12.04-BREAKROOM SNT	Report Date: 9/2/2021 1.57:42PM Eastern Standard Time Report Date: 3/2/2021 1.57:43PM Eastern Standard Time	Event Outsils	2/5/2021. Access Grented 2/5/2021. Access Grented	42:6188720 341913079	5 10
On Guard 7.4 Access Granted by Reeds: On Guard 7.4 Access Granted by Reeds	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXY-1.2:03-PKC UP RW IN, DAXY-1.2:04-BREACROOM ENT CAMERY: START DATE: 2/1/2021 12:00:00 AW; END DATE: 2/2/2021 11:59:59 PM; READERS: DAXY-1.2:03-PKC UP RW IN, DAXY-1.2:04-BREACROOM ENT	Report Date: 3/2/2021 157542PM Eastern Standard Time	Event Details Event Details	2/5/2021 Access Granted	341922904	11
On Buard 7.4 Access Granted by Reader	CALERY: START DATE: 3/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXT-1.2.09-PICK UP RM IN, DAXT-1.2.04-BREAKROOM ENT CALERY: START DATE: 3/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXT-1.2.09-PICK UP RM IN, DAXT-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021, 1:57:42PM Seatern Standard Time Report Date: 3/2/2021, 1:57:42PM Seatern Standard Time	Event Details Event Details	2/5/2021 Access Granted 2/5/2021 Access Granted	410492872 341889308	12 13
On Guard 7.4 Acress Granted by Reader On Guard 7.4 Acress Branted by Reader	QUERY: STAKT HATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:58 PM; READERS: OAXT-1.2.03-PICK UF RM IN; DAXT-1.2.04-GREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datalis Event Datalis	2/5/2021 Access Started 2/5/2021 Access Started	34192554B 341893414	1/1 1/5
OnGuard 7.4 Access Granted by Reede OnGuard 7.4 Access Granted by Reeds	QUERT: START DATE: 2/1/2021 12:00:00 AM; SMD DATE: 8/2/2021 11:58:59 PM; READERS: DAX7-1.2:03-PICK UP RM IN, DAX7-1.2:04-SREAKROOM ENT CUERT: START DATE: 1/1/2021 12:00:00 AM; SMD DATE: 1/2/2021 11:50:59 PM; READERS: DAX7-1.2:03-PICK UP RM IN, DAX7-1.2:04-SREAKROOM ENT	Report Date: 9/2/2021 157:42PM Eastern Standard Time Report Date: 9/2/2021 157:42PM Eastern Standard Time	Event Outsits	2/5/2021 Access Granted 2/5/2021 Access Granted	43 61 93 060	16
OnGuard 7.4 Access Granted by Reade	CALERY: START DATE: 2/1/2021 12:00:00 AM: EVD DATE: 3/2/2021 11:59:59 PM: READERS: DAX7-1, 2.03-PKCK UP RM IN, DAX7-1, 2.04-BREAKRUDM ENT	Report Date: 3/2/2021 2:57:42PM Eastern Standard Time Report Date: 3/2/3021 2:57:42PM Eastern Standard Time	Event Details Event Details	2/5/2021 Access Granted 2/5/2021 Access Granted	341920107 341890101	17 18
On Suand 7.4 Access Granted by Rande On Guard 7.4 Access Smitted by Rande	CLIERY: START DATE: 2/1/2021 12:00:00 AM: EVD DATE: 3/2/2021 11:59:50 PM; READERS: DAXX-1.2:03-PICK UP RM IN, DAXY-1.2:04-BREAKROOM ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details	2/5/2021 Access Stanced	24 1925218	19
OnGuard 7.4 Access Branted by Reade OnGuard 7.4 Access Granted by Reade	QUERT: 57ART DATE: 2/1/1021 12:00:00 AM; END DATE: 9/2/2021 11:58:58 PM; READERS: DAX7-1.2:08-RCK UP RM IN, DAX7-1.2:04-BREAKROOM ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details Event Details	2/5/2021 Access Stanted 2/5/2021 Access Stanted	496178476 341892084	20 21
Dr. Guard 7.4 Access Granted by Reads	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:50:59 PM; READERS: CAX7-1.2,03-FICK UP RIA IN, DAX7-1.2,04-BRGAIROOM ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details Event Details	2/5/2021 Access Granted 2/5/2021 Access Granted	341892767 541882569	22 23
OnGuard 7.4 Access Granted by Reads OnGuard 7.4 Access Granted by Reads	DUFRY: START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2:08-PICK UP RM IN, DAX7-1.2:08-BREAKROOM ENT	Report Oate: 3/2/2021 1:57;42PM Eastern Standard Time	Event Details	2/5/2021 Access Grented	436177838	24
OnGuard 7.4 Access Gramed by Rande	QUERY: START DATE: 2/1/1021 12:00:00 AAV; END DATE: 3/2/2021 11:59:59 PM; READERS: DASG-1.1.09-PICK UP RM IN, DASY-1.2:09-BILEARNCOM ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details Event Details	2/5/2021 Access Granted 2/5/2021 Access Granted	341910985 341922570	25 26
On Guard 7.4 Access Granted by Reads On Guard 7.4 Access Granted by Reads	QUERT: START DATE: 2/1/2021 12:00:00 AM; BND DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2.09-PICK UP RM IN, DAX7-1.2.09-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detells Event Detells	2/5/2021 Access Granted 2/5/2021 Access Granted	341892326 341925850	27 28
On Guard 7.4 Access Granted by Reads On Guard 7.4 Access Granted by Reads		Report Date: 3/2/2021 1:57:42PM Eastern Standard Three Report Date: 3/2/2021 1:57:42PM Eastern Standard Three	Event Details	2/5/2021 Access Grented	410/90093	29
On Sward 7.4 Access Granted by Reads	 QUERY: START DATE: 2/1/2011 12:00:00 AM; END DATE: 3/2/2021 11:59:58 PM; READERS: DAXY-12:05-PICK UP RM IN, DAXY-12:04-BREAKROOM ENT 	Report Outo: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Sestern Standard Time	Event Details Event Details	2/5/2021 Access Granted 2/5/2021 Access Granted	436177617 341906658	30 31
On Suand 7.4 Access Granted by Reeds On Suand 7.4 Access Granted by Reeds	 CHIERT: START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11:59:59 PM: READERS: DAXC-1,2,09-PICK LIP RM IN, DAXT-1,2,09-REAKROOM ENT 	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Ontails	2/5/2025 Access Granted 2/5/2021 Access Granted	341925284 341906470	32 33
OnGuard 7.4 Access Granted by Reads OnGuard 7.4 Access Granted by Reads		Separt Date: 3/2/2021 1:57542PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/5/2021 Access Granted	341890520	34
On Guard 7.4 Access Granted by Reads	 QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 9/2/2021 11:59:59 PM; READERS: DAX7-12.03-PKX UP RM IN, DAX7-12.04-BREAKROOM ENT 	Report Date: 3/2/2021. 1:57:42PM Eastern Standard Time Report Date: 3/2/2021. 1:57:41PM Eastern Standard Time	Event Details Event Details	2/5/2021 Access Granted 2/5/2021 Access Granted	263669595 34390666&	35 36
On Guard 7.4 Access Granted by Read On Guard 7.4 Access Granted by Read	CALERT: START DATE: 2/1/2021 12:00:00 AM: END DATE: 8/2/2021 11:59:59 PM: READERS: DAX7-1.2:09-PICK UP RM IN, DAX7-1.2:04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Drent Details	2/5/2021 Access Granted 2/5/2021 Access Granted	341922570 410432873	37 38
OnGuard 7.4 Access Strented by Read On Quard 7.4 Access Granted by Read	 CHIPPY: START DATE: 2/1/2021 12:00:00 AM: FND DATE: 2/2/2021 11:59:59 PM: READERS: QAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT 	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Ortalis	2/S/2021 Access Granted	341922970	39
On Ram rd 7.4 Access Granted by Read	W QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:59:59 PM; READERS: DAXT-12:03-PICK UP RIA IN, DAID-12:00-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Tree Report Oate: 3/2/2021 1:57:42PM Sestern Standard Time	Avent Details	2/5/2021 Access Stented 2/5/2021 Access Stanted	843890498 410428955	40 41
On Guard 7.4 Access Granted by Read DinGuard 7.4 Access Granted by Read	M QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:50 PM; READERS: DAW-1.2.03-PICK UP RM IN, DAYT-1.2.04-REAKTCOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Titue	Event Datells	2/5/2021. Access Granted	241818864 251336739	41 49
On Suard 7.4 Access Branted by Read On Suard 7.4 Access Granted by Read	W CALERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/4/2021 11:59:59 PM; READERS: DAG-1-2:09:PICK UP RM IN, DAVE-3-2:DI-BESAKBODM ENT	Report Date: 1/2/2021 1:57:43PM Eastern Standard Time Report Date: 3/2/2021 1:57:43PM Eastern Standard Time	Event Details Event Details	2/5/2021 Access Smitted 2/5/2021 Access Smitted	410430031	44
On Sugard 7.4 Acoms Granted by Read	■ DJERY: START DATE: 2/1/XVI 1 12:0000 AM: END DATE: 3/2/XVI 11:50:59 PM: READERS: DAX7-1,2.09-PKX UP RM IN, DAX7-1,2.04-BREAKROOM ENT	Report Date: 9/2/2021 1:57:42PM Eastern Standard Time Report Date: 9/2/2021 1:57:42PM Seatern Standard Time	Event Details	2/5/2021 Access Granted 2/5/2021 Access Granted	158989741	45 46
OnGuard 7.4 Acress Granted by Read OnGuard 7.4 Acress Granted by Read	OLIENY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXY-1.2.09-PICK UP RM IN, DAXY-1.2.09-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Seatern Standard Time	Event Datells	2/5/2021, Access Granted	341911449 341925923	47 48
On Guard 7.4 Access Granted by Rand On Guard 7.4 Access Granted by Rand	COURTY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DATG-12:03-PICK UP RM IN, DATG-12:03-BIEAKROGIM ENT	Report Date: a/z/2021 1:57:12PM Eastern Standard Time Report Date: a/z/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/5/2021 Access Granted 2/5/2021 Access Granted	251309140	49
On Buard 7.4 Access Granted by Read	EF QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:59:59 PM; READERS: DAX7-12:03-FCK UF RM IN, DAX7-12:03-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detalls Event Details	2/5/2021 Access Granted 2/5/2021 Access Granted	341906668	50 51
OnGuerd 7.A Access Granted by Reso OnGuerd 7.A Access Granted by Reso		Report Data: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/8/2021 Access Grented	436177793	5
On Sward 7.4 Access Granted by Rose	CUERY: STAIT DATE: 2/1/2021 32:00:00 AM; END DATE: 1/2/2021 11:59:59 PM; READERS: DAX7-1.2.08-PICK UP RM; N, DAX7-1.2.04-BIREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/6/2021 Access Granted 2/6/2021 Access Granted	341913387 341891665	3
OnGuard 7.4 Access Granted by Read OnGuard 7.4 Access Granted by Read	BY QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:58:59 PM; READERS: DAXY-1.2.03-PICK UP RIM IN, DAXY-1.2.04-BREADEROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/6/2021 Access Greated 2/6/2021 Access Branted	436174709 341921803	4
On Guard 7.4 Access Granted by Reas On Guard 7.4 Access Granted by Reas		Report Unto: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/8/2021 Access Grented	341892082	6
On Street 7.4 Acress Street In Sec.	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:59:59 PM; REQUES: DATG-12:02-PDC UP RM RQ, DATG-12:04-BREAKROOM ENT BY QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:59:59 PM; REQUES: DATG-12:03-PDCK UP RM IN; DATG-12:04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Fastern Standard Time Report Date: 3/2/2021 1:57:42PM Earlern Standard Time	Event Details Event Details	2/6/2021 Access Granted 2/6/2021 Access Granted	456188720 341892767	8
OnGuard 7.4 Access Granted by Resi	LA CHERT CATE 2/1/2011 12:00:00 AM: END DATE: 3/2/2021 11:59:59 PM: READERS: DAZO-1.2:09-PM: UP RM IN, DAXO-1.2:04-6READROOM ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Detella Event Detella	2/6/2021 Acoust Granted 2/6/2021 Acoust Granted	341925272 436174445	70 a
On Guard 7.4 Access Granted by Ree On Guard 7.4 Access Granted by Ree	INFO COURTY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXY-12.03-PKX OF NAMIN, DAXY-12.03-BREADAMOON ENT	Report Date; 3/2/2021 1:57:42PM Eastern Standard Time	Brant Datalle	2/6/2021 Access Branted	341908144	11
On Bug of 7.4 Access Grented by Ree Continued 7.4 Access Granted by Ree	INF QUERY; START DATE: 2/1/2021 12:00:00 AM; SHD DATE: 2/2/2021 11:50:50 PM; READERS: DAX7-1,2.03-PICK UP RM IN, DAX7-1,2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57/42PM Eastern Standard Time Report Date: 3/2/2021 1:57/42PM Eastern Standard Time	Event Details Event Details	2/6/2021. Access Granted 2/6/2021. Access Granted	341912991 341892282	12 12
Opguard 7.4 Access Granted by Ree	OLERY: START DATE: 2/1/2021 12:0000 AM: END DATE: 3/1/2021 11:59:59 PM: READERS: DAXG-1.2.09-PICK UF RM IN, DAX7-1.2.04-BREAKROOM ENF	Report Detai: 3/2/2021 1:57:42PM Eastern Standard Hree	Event (lejalls Event Deialls	2/6/2021 Access Granted 2/6/2021 Access Granted	436 178476 341913079	14 15
On Suand 7.4 Access Granted by Rea On Guard 7.4 Access Granted by Rea	Ser QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:50 PM; READERS: DAXT-1.2.03-FICK UP RM IN, DAXT-1.2.04-BREAKRICINA EN	Report Date: 5/2/2021 1:57:42PM Eastern Stendard Time	Event Details	2/6/2021 Access Grented	34 1891093 43-6193060	18 17
OnGuard 7.4 Access Granted by Rea OnGuard 7.4 Access Granted by Rea	BIT QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DARY-1.2.03-PICK UP RM IN, CACY-1.2.03-BIEARROOM ENT	Report Date: 3/2/2021 1:57942PM Eastern Standard Time	Event Datella Event Datella	2/6/2021 Access Granted 2/6/2021 Access Granted	341922504	18
On Sweed 7.4 Access Grented by Rea	HE QUERY; START DATE: 2/1/2021 3250H30 AM; END DATE: 3/2/X021 31:59:59 PM; READERS: DAX7-1.2.03-PKX UP RM IN, DAX7-1.2.04-BREAKROOM EN	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/6/2021 Access Granted 2/6/2021 Access Granted	34762548	19 20
On Suppl 7.4 Access Granted by Rea On Guard 7.4 Access Granted by Rea	HER CHERY STATT DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11:59:59 FM: READERS: DAX7-1.2.03-PICK UP RM (N, DAX7-1.2.03-BREAKRODM EN	Report Outs: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	1/6/2021 Access Grenned	436174401 341820107	21
OnGuard 7.4 Access Granted by Red OnGuard 7.4 Access Stanted by Red	der Query: Start date: 2/1/2021 12:00:00 am; end date: 3/1/2021 11:55:50 pm; readers: daxy-1.2.03-pick up rm in, daxy-1.2.04-breakboom en der Cuery: Start date: 2/1/2021 12:00:00 am; end date: 3/1/2021 12:58:50 pm; readers: daxy-1.2.03-pick up rm in, daxy-1.2.04-breakboom en	Report Date: 3/2/2021 1;57942PM Eastern Standard One Report Date: 8/2/2021 1:57942PM Eastern Standard Time	Event Details	2/6/2021 Acress Granted 2/6/2021 Acress Granted	341912682	72 23
OnGuard 7.4 Access Granted by Rac	Her DUERY: START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/1/2021 11:59:58 PM; READERS: DAX7-1.2:09-PIO: UP RM IN, DAX7-1.2:04-BREAKROOM EN	Report Date: 3/3/2021 157:43PM Eastern Stringera (ime	Event Details Event Details	2/8/2021 Access Granted 2/6/2021 Access Granted	3418893 C8 343528627	24 25
On Guard 7.4 Access Granted by Res On Guard 7.4 Access Granted by Res	HER CHIERY STANT CATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11:59:59 PM: READERS: DAX7-1,7,02-PICKUP RM IN, DAR7-1,2,04-BREAKROOM EN	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/6/2021 Access Granted 2/6/2021 Access Granted	456174791 341975218	26 27
On Swent 7.4 Access Granted by Re- On Swent 7.4 Access Granted by Re-		T Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/G/2021 Access Granted	341919644	28
ConGuerd 7.4 Access Granted by Re-	die Query: Start Date: 2/1/2021 12:00:00 AM; END DATE: 9/2/2021 11:59:59 PM; READERS: DATO-1.1:09-PKX UP RM (M, DATO-1.2:04-BREADROOM EN	7 Asport Date: 3/2/2021, 1:57:A2PM Eastern Standard Time	Event Details Event Details	2/6/2021 Access Crented 2/6/2021 Access Grented	341892569 341852084	29 30
OnGuard 7.4 Access Granted by Res OnGuard 7.4 Access Stanted by Res	HER CHERY: STAUT DATE: 2/1/2011 12:00:00 AM: END DATE: 3/2/2021 11:50:50 PM: READERS: DAX7-1, 2,05-PICK UP RM IN, DAX7-1, 2,04-8 KEAGROOM EN	F Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/6/2021 Access Granted 2/6/2021 Access Granted	341915622 341891583	31 32
On Busing 7.4 Access Granted by Ra- On Busing 7.4 Access Granted by Ra-	der Olery; Start Date: 2/1/2021 12:00:00 AM; END Date: 3/2/2021 11:59:59 PM; READERS: DAX7-1,2:08-PICK UP RM IN, DAX7-1,2:04-REAXROOM EN OLERY; START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1,2:08-PICK UP RM IN, DAX7-1,2:04-REAXBOOM EN	T Report Date: 3/2/2021 157:62FM EARER Standard Time	Event Details	2/6/2021 Access Granted	410032879	35
OnGuard 7.4 Access Granted by Re	IGOT QUERY: START DATE: 2/1/2021 12:00:00 AM; EMD DATE: 1/2/2021 11:59:59 PM; READERS: DAX7-1.2:09-PKX UP RM IN, DAX7-1.1:04-BREAKROOM EN	Report Date: 3/2/2021 1:57:42PM Sectors Standard Time	Event Details Event Details	2/6/2021 Access Granted 2/6/2021 Access Granted	496177838 486174665	34 35
OnGuard 7.4 Access Granted by Re OnGuard 7.4 Access Granted by Re	Idea - CUERY: START DATE: 2/1/2021 12:00:00 AM: END ORTE: 3/2/2021 11:58:59 PM: READERS: DAXT-1.2/03-PHX UP RM IN, DAXT-1.2/03-BREAKROOM EN	Report Date: 11/2/2021 1:57:42PM Eistern Standard Time	Event Details	2/6/2021 Access Granted 2/6/2021 Access Granted	341691731 341892525	36 97
On Guard 7.4 Access Granted by Re ConGuard 7.4 Access Granted by Re	DUERY; START DATE: 2/1/2021 12:0000 AM; END DATE: 2/2/2021 11:58:59 PM; REPUBES: DAX7-12:03-PICK UP RM IN, DAX7-12:04-BREAKROOM EN	T Report Date: 8/2/2021 1:57:43PM Eastern Standard Time T Report Date: 8/2/2021 1:57:42FM Eastern Standard Time	Event Details Event Details	2/6/2021 Access Granted	341921970	85
On Quard 7.4 Access Granted by Re	USBY: \$TART DATE: 2/1/2021 12:00:00 A/-, END DATE: 1/2/2021 11:59:59 PM; READERS: DAM:-1.2.09-PICK UP RM: IN, DAX7-1.2.04-EREAKE/JOAN EP	IT Report Date: 3/2/2021 1:57:42FM Eastern Standard Time	Event Details	2/6/2021. Access Granted 2/6/2023. Access Granted	341925350 486174379	39 40
On Guard 7.4 Access Granted by Re On Guard 7.4 Access Granted by Re	ouery: Start Date: 7/1/2021, 12:00:00 AM; END Date: 3/2/2021 11:59:59 PM; READERS: DAXT-1.2.03-PICK UP RM:N, DAXT-1.2.01-BREAXROOM BY	Report Date: 3/7/2021, 1:57:42914 Eastern Standard Time	Event Detalls	2/6/2021 Access Granted	43.5192950 261.669440	41 42
OnGuard 7.4 Access Granted by Re OnSuerd 7.4 Access Granted by Re	ABER QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:50:59 PM; READERS: DAXT-1.2.03-PICK UP RM IN, DAXT-1.2.04-BREADERS: DAXT-1.2.04-BREADERS	If Report Date: 3/2/2021 1:57:42PM Eastern Standard Time (T Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datelia	2/6/2021 Access Granted 2/6/2021 Access Granted	341906668	43
On@used 7.4 Access Granted by Re	where OUERY: START DATE: 2/1/2021 12:00:00 AM; SND DATE: 3/2/2021 11:59:55 FM; READERS: DAIG-1,2:09-PICK UP RM IN, DAX?-1.2.04-BREADROOM EI	IT Report Date: 3/2/2021 1:57:42FM Restern Standard Time		2/6/2021 Access Granted 2/6/2021 Access Granted	341925284 341930986	44 45
On@used 7.4 Access Granted by Ru On@used 7.4 Access Granted by Ru	eder CLERY: START DATE: 2/1/20/112:00:00 AM; END DATE: 3/1/20/2111:59:59 PM; READERS: DAX7-1.2.03-PICK UP RM IN, CAX7-1.2.04-BREADROOM B	IT Report Date: 3/2/2021 1:57:42PM Emisers Standard Time	Everyt Details	2/6/2021 Access Granted 2/6/2021 Access Granted	261699595 341906668	46 47
OnGuard 7.4 Access Granted by Re OnGuard 7.4 Access Branted by Re	QUENT: START DATE: 3/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:59:59 PM; READERS: DATE-1.2.08-PICK UP RM IN, DATE-1.2.08-BREAKROOM ST QUENT: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:59:59 PM; READERS: DATE-1.2.08-PICK UP RM IN, DATE-1.2.08-BREAKROOM ST	NT Report Date: 3/3/2021, 1;57:42PM Eastern Standard Time NT Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Detalls	2/6/2021 Access Granted	341921803	48
OnSwert 7.4 Access Granted by Re	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 9/3/2021 11:59:59 PM; READERS: DAXG-1.2.03-PICK UP RM IN, DAXG-1.2.04-PREADERSOAM	4T Report Date: 3/2/2021, 1:579GPM Eastern Standard Time	Event Details Event Details	2/6/2021 Access Brented 2/6/2021 Access Granted	410432873 456174791	49 50
OnGuard 7.A Access Granted by Re OnGuard 7.A Access Granted by Re	CUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:00:59 PM; RÉNDERS: CAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-GREANNOOM B	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Salells	2/6/2021 Access Granted 2/6/2021 Access Granted	341592767 251234728	51 52
CinGuard 7.4 Access Granted by R OnGuard 7.4 Access Granted by R	NIGHT CUERY: START DATE: 2/1/2021 12:00:00 AM; GND DATE: 1/2/2021 11:59:59 PM; HEADERS; DACT-12:09-PICK UP RM IN, DACT-12:00-BREADERS OM E UMBER CUERY; START DATE: 2/1/2021 12:00:00 AM; BND DATE: 3/2/2021 11:59:59 PM; READERS; DACT-12:00-PICK UP RM IN, DACT-12:04-BREADERS OM E	NT Report Date: 3/2/2021 3:57/92*M Eastern Standard Time NT Report Date: 3/2/2021 3:57/92*M Eastern Standard Time	Event Details	1/6/2021 Access Grantes	34180498	53
OnGuard 7.4 Access Granted by R	INDER CHERTY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAUG-11:09-PICK UP SIM IN, DAX7-12:09-BREADROCOM E	NT Report Date: 3/2/2021 1:57:429M Eastern Standard Time	Event Detells	2/6/2021 Access Grenter 2/6/2021 Access Grenter	410429965 341919864	54 55
On Suand 7.4 Access Sherted by R On Suand 7.4 Access Scanted by R				Z/6/2021 Access Grantso	341889639	58

On Bound 7.A. Access Special by Revolve QUENT: START DATE: 2/1/2021 12:00:00 AM; BIO DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2.CS-PICK UP RM IN, DAX7-1.2.04-READERSON ENT		vent Datalis vent Datalis	2/6/2021 Access Stemed 2/6/2021 Access Stemed	341906470 341927090	57 58
On Burnel 7.4 Access Granted by Freeder CUSR'S START DATE: \$7/2/021 12:0000 AM; END DATE: \$7/2/021 11:59559 PM: READERS: DATE: 1-1.203 FTC, UP RN IN, UNITAL LUC-SPEANING OF ENTIRE PM CONTROL TO A CUSPIN START DATE: \$7/2/021 11:59559 PM: READERS: DATE: 1-1.203 FTC, UP RN IN, DATE: 1-1.204 FTC, UP RN IN, DATE: 1-1.203 FTC, UP RN IN, DA	Report Cate: 3/2/2021 2:57:42PM Sestern Standard Time 1	vant Details	2/6/2021 Access Granted 2/6/2021 Access Granted	341911449 341925923	59 60
Enguero 7.4 Access Granted by Render QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READ BIS: BAXT-12:03-PKX UP BM IN, DAX7-12:04-BRANCOWNEN		vent Details vent Details	2/6/2021 Access Grented	219090979	61
CHOUNTS TAL Access Granted by Reader QUERT: START DATE: 2/1/2021 11:0000 AM; SHO DATE: 3/1/2021 11:00:00 PM; READERS: DATE-1,2/05-MCC UP RM IN, DATG-1,2-04-READEROOM SVT CHORUNG TAL Access Granted by Reader QUERT: START DATE: 2/1/2021 12:00:00 AM; SHO DATE: 3/1/2021 11:00:00 PM; READERS: DATG-1,2-05-MCC UP RM IN, DATG-1,2-04-READEROOM SVT CHORUNG TAL ACCESS GRANTED BY READERS: DATG-1,2-05-MCC UP RM IN, DATG-1,2-04-READEROOM SVT CHORUNG TAL ACCESS GRANTED BY READERS: DATG-1,2-05-MCC UP RM IN, DATG-1,2-04-READEROOM SVT CHORUNG TAL ACCESS GRANTED BY READERS: DATG-1,2-05-MCC UP RM IN, DATG-1,2-04-READEROOM SVT CHORUNG TAL ACCESS GRANTED BY READERS: DATG-1,2-05-MCC UP RM IN, DATG-1,2-04-READEROOM SVT CHORUNG TAL ACCESS GRANTED BY READERS: DATG-1,2-05-MCC UP RM IN, DATG-1,2-04-READEROOM SVT CHORUNG TAL ACCESS GRANTED BY READERS: DATG-1,2-05-MCC UP RM IN, DATG-1,2-04-READEROOM SVT CHORUNG TAL ACCESS GRANTED BY READERS: DATG-1,2-05-MCC UP RM IN, DATG-1,2-04-READEROOM SVT CHORUNG TAL ACCESS GRANTED BY READERS: DATG-1,2-05-MCC UP RM IN, DATG-1,2-04-READEROOM SVT CHORUNG TAL ACCESS GRANTED BY READERS GRANTED	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	vent Detells vent Details	2/6/2021 Access Branted 2/6/2021 Access Granted	251309140 341906668	62
On Guard 7.4 Access Granded by Resider QUERY: START DATE: 2/1/2021 12/5000 AM; EMD NATE: 2/1/2021 12/5009 PM; READERS: DATE: 1.209-PM; UR READERS: DATE: 1.209-PM; UR READERS: DATE: 1.209-PM; UR READERS: DATE: 1.209-PM; DATE: 2.099-PM; DATE: 2.099-PM; DATE: 2/1/2021 12/5009 PM; EMD READERS: DATE: 1.200-PM; DATE: 2.099-PM; DATE: 2.099	Report Date: 8/2/2021 1:57:42PM Eastern Standard Time	iveni Details	2/6/2021 Access Granted	3418899EB	64
On Granted 7.4 Access Strented by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DACT-1.2:03-FICK UP RHININ, DATE: 1/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DACT-1.2:03-FICK UP RHININ, DATE: 1/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DACT-1.2:03-FICK UP RHININ, DATE: 1/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DACT-1.2:03-FICK UP RHININ, DATE: 1/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DACT-1.2:03-FICK UP RHININ, DATE: 1/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DACT-1.2:03-FICK UP RHININ, DATE: 1/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DACT-1.2:03-FICK UP RHININ, DATE: 1/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DACT-1.2:03-FICK UP RHININ, DATE: 1/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DACT-1.2:03-FICK UP RHININ, DATE: 1/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DACT-1.2:03-FICK UP RHININ, DATE: 1/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DATE: 1/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DATE: 1/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DATE: 1/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DATE: 1/1/2021 12:00:00 AM; END DATE: 1/1/2021 12:00 AM; END		ivent Details Seent Details	2/7/2021 Access Granted 2/7/2021 Access Granted	341913387 341913189	2
OnGuard 7.A. Access Granted by Render QUERY: \$148T DATE: 2/1/2021 12:00:00 AN; BN DATE: 3/1/2021 13:20:52 PM; BEADERS: DAXY-1_2-2-5-FM; DAXY-1_2-5-FM; DAXY	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	ivent Cotalis Event Duteis	2/7/2021. Access Granted 2/7/2021. Access Granted	341921803 341892062	3 4
On Sharid 7.4 Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; BEDDES: DAX-1.2:09-FM IN,	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detaus	2/7/2021 Access Granted	3419 19556	5
On Guard 7.4 Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 12:59:59 PM; READERS: DAXT-1.2:05-PKX UP RIAI IN, DAXT-1.2:04-PKX ARCHIVE START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 12:59:59 PM; READERS: DAXT-1.2:05-PKX UP RIAI IN, DAXT-1.2:04-PKX ARCHIVE START DATE: 2/1/2021 12:50:50 PM; END DATE: 3/2/2021 12:59:59 PM; READERS: DAXT-1.2:05-PKX UP RIAI IN, DAXT-1.2:05-PKX UP R	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	brent Details Event Dotalis	2/7/2021 Access Granted 2/7/2021 Access Granted	436177795 341912991	6 7
On Guard 7.4 Azam Granted by Reader QUERY: START DATE: 2/1/2011 12:0000 AN; BND DATE: 2/1/2011 12:0000 AN; BND DATE: 2/1/2011 12:0509 AN; READERS: DATA-LAUSE PLOT UP BN IN, DATA-LOU-BREAKROUM ENT	Report Date: 3/7/2021 1:57:929M Eastern Standard Time	Event Details	2/7/2021 Access Streeted 2/7/2021 Access Streeted	341925548 341925872	8
On Suprict 7.4 Access Stretted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXT-1.2.09-PICK UP RELIED DATE: 1/2/2021 11:59 PM;	Report Date: 3/2/2021 1:57:42FM Fintum Standard Time	Event Details	2/7/2021 Access Granted	436174445	10
A AND DESCRIPTION OF THE PROPERTY OF PARTY IN THE PARTY I	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/7/2021 Access Granted 2/7/2021 Access Granted	341908254 341892252	11 12
Dr.Guard 7.4 Accest Granted by Reader DLERY: START DATE: 2/1/2021 12:0000 AM; END DATE: 2/1/2021 11:05:00 PM; ReaderS: DACY-1-2-01-PICCU PM IN, DA	Report Date: 3/2/2021 3:57:42PM Eastern Standard Time	Event Detells	2/7/2021 Access Brented 7/7/2021 Access Granted	341924226 456174709	13 14
On Stuard 7.4 Access Grented by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 5/2/2021 11:59:59 PM; READERS: DAXX-1.2.05-PICE UP RM IV, GAX/-1.2.05-PICE UP RM IV, GAX/-1.2	Report Date: 3/2/2021 1:57;42PM Eastern Standard Time	Event Detells Event Detells	2/7/2023 Access Granted	341920107	15 16
Origand 7.4 Access Granted by Reader QUERY: \$TAR7 DATE: 2/1/2021 12:00:00 AM; END DATE: 2/2/2021 11:59:50 FM; READERS: DAXY-1.204-READERS: DAXY-1.		Event Detalls Event Detalls	2/7/2021 Access Granted 2/7/2021 Access Granted	2418E1099 24191E544	17
On Stated 7.4 Access Granted by Bander OLERY: \$7347 OATS: 2/1/2001 12:00:00 AM; FIND DATE: 3/1/2001 13:00:00 PM; BEADERS: DAY-1-1.00-PHCK UP RM IN) DAY-1-2.04-BEADEROOM ENT	Report Cate: 3/1/2011 1:57:42FM Eastern Standard Time	Event Detells Event Detells	2/7/2021 Access Granted 2/7/2021 Access Granted	34192496 341892569	18 19
On Bluerd 7.4 Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:59:59 PM; READERS: DAXO-1.2:09-RICK UP RMAIN, DAXY-1.2:04-RIMAN DAXY-1.2	Report Date: 3/1/2021 1:57:42PM Eastern Standard Time	Event Details	2/7/3021 Accass Granted	436174401	20
THE PARTY NAME OF THE PARTY NA	Report Date: 3/2/2021 1:57:42PM Eastern Standard 78ths	Event Datells Event Datells	2/7/2021 Access Scanted 2/7/2021 Access Stanted	241919522 341891533	21
OnStant 7.4 Access Pennind to Resolve QUERY, STANT DATE: 2/1/2012 12:20:000 AAV; FIND DATE: 3/2/2012 11:35:50 PM; READERS: DATE: 2/1/2012 10:35:50 PM; READERS:	Report Deta: 3/2/2021 1:57:42FM Certain Standard Time	Event Details	2/7/3021 Acress Granted 2/7/3021 Access Granted	341913409 341926627	23 24
Originated 7.4 Access Granded by Reader: QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/1/2021 13:59:25 PM; READERS: DAXX-1.2:00-3TICK UP RIM N/N, DAXX-1.2:00-4READEROUNE ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details	2/7/2021 Access Granted	426174731	25
	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/7/2021 Access Granted 2/7/2021 Access Granted	341924094 341925218	26 27
On Guard 7.4 Access Sentral by Reader QUERY: STATE DATE: 2/1/2021 12:90:00 AM; PAD DATE: 3/2/2021 11:59:59 PM; READERS: DATE-12/20-PECF BY RIN, DATE-12-QUERNARD AND DATE-12/20-PECF BY RIN, DATE-12-QUERNARD AND DATE-12/20-PECF BY RINGS BY	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/7/2021 Acress Granted 2/7/2021 Access Granted	430433710	25 29
On Gazard 7.4 Access Granted by Reader CLEEY: START DATE: 2/1/2021 12:00:00 AAH; BND DATE: 3/7/2021 12:00:00 AAH; BND DATE: 3/7/2021 12:00:00 AAH; BND DATE: 3/7/2021 12:00 AAH; BND DATE:	Report Date: 1/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Datails	2/7/2021 Access Granted	410429833	30
CONTROL OF THE PARTY OF THE PAR	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datalis Event Datalis	2/7/2021 Access Granted 2/7/2021 Access Granted	42G178476 341892D84	31 32
OnGuerd 7.4 Access Granted by Reader DLERY: STATAT DATE: 3/1/2011 12:00:00 AM; END DATE: 3/1/2011 11:56:59 PM; BEADER: DATY-1.1:00-PFC UP RIM IN, DAY-1.2:00-REARROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/7/2021 Access Granted	410432872	39 24
On Shard 7.4 Access Granted by Rander QUERY: START DATE: 1/1/2021 12:00:00 AM; SND DATE: 3/2/2021 11:59:59 FM; READERS: DADO-1,2:03-PM; READERS: D	Report Date: 3/2/2021 3:57:42PM Eastern Standard Time Report Oate: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/7/2021 Access Granted 2/7/2021 Access Granted	341110986	35
TABLE DATE: 2/1/2003 (2:00:00 and: FATT) 2/1/	Report Date: 9/2/2021 1:57:42PM Septem Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Octalis	2/7/2021 Access Granted 2/7/2021 Access Granted	341925350	36 37
DriSurri 7.4 Access Granted by Reside CUERY: START DATE: 2/1/2021 13:00:00 AAR; DRID DATE: 1/1/2021 13:59:59 PM; READERS: DAD/-13.20-PICK UP ARM IN, DATE: 1/1/2021 13:59:59 PM; READERS: DAD/-13.20-PICK UP ARM IN A	Report Date: 3/2/2021, 2:57:40PM Septem Standard Time	Event Details	2/7/2021 Access Granted	341924402 436183060	38 29
	Report Date: 3/2/20X1 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/7/2021 Access Granted 2/7/2023 Access Granted	289219546	40
OnGuard 7.A. Access Granted by Installer ConGuard 7.A. Access Grant	Report Date: 3/2/2021 1:57:43PM Feature Standard Time Report Date: 3/2/2021 1:57:43PM Eastern Standard Time	Event Details Event Details	2/7/2021. Access Grented 2/7/2021. Access Grented	289219546 436174379	41
On Guard 7.4 Access Granted by Resider CULERY: STAT DATE: 3/7/20112-0000 ANE; 980 DATE: 3/7/201113-05-99 PM; READIRES: DAZY-1_2.06-9/CKU PRIM IN, DAZY-1_2.04-9/EMBORDOM ENT	Report Date: 3/2/2021 1:57:43PM Eastern Standard Time	Event Details	2/7/2021 Access Granted	410429948	43
OriGuard 2.4 Acoust Granted by Reader QUERY: STARY DATE: 2/1/2021/12:00:00 AM; PRO DATE: 5/2/2011/15:95-95 PM; READ PROTE DATE: 1/1/2021/12:00:00 AM; PRO DATE: 5/2/2011/15:95-95 PM; READ PROTE DATE: 1/1/2021/12:00:00 AM; PRO DATE: 5/2/2011/15:95-95 PM; READ PROTE DATE: 1/1/2021/12:00:00 AM; PRO DATE: 5/2/2021/12:00:00 AM; PRO DATE: 5/2/2021/12:00 AM; PRO DATE: 5/2/2	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 1/2/2023, 1:57:42PM Eastern Standard Time	Event Details Event Details	2/7/2021. Access Granted 2/7/2021. Access Granted	410430031	45
Originary 2.A. Access common by Memoiar QUERY: START DATE: 2/1/2021 12:00:00 AM; DIO DATE: 3/1/2021 11:00:00 PM; READERS: DAXY-1.2.03-PMCK UP RM IN, DAXY-1.2.04-RIBANDOOM ENT DISBURG 7.A. Access Common by Memoiar QUERY: START DATE: 2/1/2021 12:00:00 AM; DIO DATE: 3/1/2021 15:95:20 PM; READERS: DAXY-1.2.03-PCK UP RM IN, DAXY-1.2.04-RIBANDOOM ENT	Report Date: 3/2/2021, 1:57:42PM Fastern Standard Time Report Date: 3/2/2021, 1:57:42PM Fastern Standard Time	Event Octalls Event Datalls	2/7/2021 Access Grented 2/7/2021 Access Granted	341884554 235836648	46 47
OnGUIRD 7.4 Science Sharked by Beeder GUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS; DAGS-12:03-PICK UP RM IN, DA	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/7/2021 Access Granced	341906668	48.
Oxioueri 7.4 Access Granded by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXY-12:03-PKX DF RM IN, DAXY-12:04-PREADERDOWN ENT	Report Date: 3/2/2021 1:57:42PM Destern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/7/2021 Access Granted 2/7/2021 Access Granted	341925284 341891665	49 50
OnSwerd 7.4 Access Grented by Rander QUERY: START DATE: 2//2/2021 12:0000 AM; EMPLOY(E: 4//2/2021 12:0000 AM; EMPLOY(E: 4//2/2	Report Date: 3/2/2023 1:57:42PM Eastern Standard Time	Event Details Reent Details	2/7/2021 Access Granted 2/7/2021 Access Stanted	341920239 341924358	51 52
On Sound 7.4 Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2011 11:59:59 PM; READERS: DAX7-12.03-PICK UP RM IN, DR07-12.10-PICK UP RM IN, DR	Report Date: 9/2/2021 2:57:42FM Eartern Standard Time Report Date: 2/2/2021 2:57:42FM Eartern Standard Time	Event Details	2/7/2021 Access Granted	436174758	[3]
THE PARTY OF THE P	Report Date: 2/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/7/2021 Access Granted 2/7/2021 Access Granted	410432872 426192550	54 55
OnStand 7.4 Access Granted by Sender QUERY: STANT DATE: 2/1/2021 12:0000 AND 1819 12:0001 11:0000 PM I READERS: DATY-11.00 PM IN QUART-11.00 PM IN QUART-11.	Report Oate: 3/2/2021 1:57:42PM Eastern Standard Time:	Event Details Event Details	2/7/2021 Acons Granted 2/7/2021 Acons Granted	341891952 342884594	56 57
OnSuard 7.4 Access Graphed by Seader QUENY: START DATE: 1/1/2021 12:00000 ANI; 19/1/2021 12:0000 ANI; 19/1/2021 12:0000 ANI; 19/1/2021 12:00000 ANI; 19/1/2021 12:00000 ANI; 19/1/2021 12:00000 ANI; 19/1/2021 12:00000 ANI; 19/1/2021 12:0000 ANI	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Sestem Standard Time	Evant Detells	2/7/2021 Access Granted	43/8174758	5h 58
Dridgerd 7.4 Access Granted by Rauder QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2:04-REX UP RM RI, DAX7-1.2:04-READERS	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/7/2021 Accres Granted 2/7/2021 Accres Granted	456197850 175916200	60
On Quart 7.4 Recess Granted by Reader QUERTY START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/1/2021 13:00 DATE: 2/1/2021 23:00 DATE: 2/1/2021 13:00 DATE: 2/1/2021 13:00 DATE: 2/1/2021 23:00 DATE: 2/1/	Report Date: 3/2/2021 1:57:42PM Eastern Standard Tirro	Syent Datails Event Datails	2/7/2021 Access Granted 2/7/2021 Access Granted	134691775 251334739	61 62
Ordinard 7.4 Access Straton by Rander DUCRY: START DATE: \$\frac{1}{2}\sqrt{2}	Report Date: 9/2/2021 1:57:42PM Eastern Standard Time Report Date: 9/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/7/2021 Acous Grented	341889689	63
24 CONTRACTOR OF THE PARTY OF T	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/7/2021. Access Granted 2/7/2021. Access Granted	341911482 341919864	64 65
On-Stated T.A. Recosts Granted by Resider CLERKY: STATE DATE: 4/1/2021 12:00:000 JAM; EMD DATE: 9/1/2021 12:000 JAM; EMD DATE: 9/1/2021 12:00:000 JAM; EMD DATE: 9/1/2021 12:000 JAM; EMD DATE	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Octobs Event Details	2/7/2021 Access Granted 2/7/2021 Access Granted	341906470 410430081	56 67
On Sured 7.4 Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AN; 9NO DATE: 1/2/2021 11:90:59 PM; READERS: 00/07-12/20-PKC UP RAN IN, DATE: 1/2/2021 12:00:00 AN; 9NO DATE: 1/2/2021 11:90:59 PM; READERS: 00/07-12/20-PKC UP RAN IN, DATE: 1/2/2	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/7/2021. Access Scented	261672745	61
THE PARTY OF THE P	Report Date: 3/2/2021 1:57/42PM Sestem Standard Time Report Date: 2/2/2021 1:57/42PM Sestem Standard Time	Event Datails Drant Details	2/7/2021 Access Stanted 2/7/2021 Access Granted	341927090 34192582a	59 70
On Quard 2.4 Access Greeked by Render LIGHTY: \$TANT DATE: \$\frac{1}{2}\triangle 22.000.00 And; \$\text{ PM}\$ DATE: \$\frac{1}{2}\triangle 22.000	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Eyervit Details	2/7/2023. Access Granted 2/7/2023. Access Granted	241911449 218611830	71.
Optioned 7.4 Access Greenand by Reeder QUERY: START DATE: 2/1/2012 12:00:00 AM; END DATE: 3/2/2021 11:59:59 FM; READERS: DAXT-12.09-91CK UP IM IN, DAXT-12.09-BREARFOOM ENI	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/7/2021 Access Granted	241906668	73
TANK OF THE PARTY	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Seport Date: 2/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/7/2021 Access Grented 2/7/2021 Access Grented	341891027 341889603	74 75
On State 17.4 Access Started by Reader QUERY: START DATE: 2/1/201112:00000 AND: 37/2013 115:859 PM; REQUEROS: IDAC-1.2.0-9-CK, UP WIN IN, DOCUMENT OF THE PROPERTY OF THE PROP	Report Cate: a/2/2021 1:57:52PM Eastern Standard Time	Event Detells Svent Detells	2/7/2021. Access Granted 2/7/2021. Access Granted	289222454 251309052	76 77
On Guerd 7.4 Access Granted by Reader QUERY; START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXY-12:00-FICK UP RMINI, DAYY-12:00-REQUIRED SMT.	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details	2/7/2021 Access Granted	249219546	78
THE REPORT OF THE PROPERTY OF	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/8/2023 Access Granted 2/8/2023 Access Granted	159466043 426177753	1 2
OnBlast 7.4 Access General by Reader QUERT: \$7540 DATE: \$757003 1150000 AM; END DATE: \$757003 1150509 PM; READERS; DAYS-1.2.00-PICK UP RAW, BUCK-1.2.00-PICK UP RAW, BUCK-1	Report Date: 3/2/2021 1:57:42PM Sastem Standard Tires	Event Details	2/8/2021 Access Granted 2/8/2021 Access Granted	410433966 341892062	2
On Guard 7.A. Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; SND DATE: 1/2/2021 11:59:59 PM; READERS: CAX7-12:20-9PCX UP RIM NJ, DAR7-12:00-ARRAGENE ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 2:57:42PM Eastern Standard Time	Event Details Event Details	2/8/2021 Acons Granted	541890454	5
A A STATE OF THE PROPERTY OF T	Report Date: 2/2/2021, 1:57;42PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Sestem Standard Time	Event Details Event Details	2/8/2021 Access Grented 2/8/2021 Access Grented	341930360 341919556	7
On-Shared T-A. Access Sentended by Reador's CALESTYS STATE COLUMN	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/8/2021 Access Granted 2/8/2021 Access Stanted	341890233 241920362	8
OrGuard 7.6 Access Granted by Render OUERY; START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:50:59 PM; REPORTS; DATE: 1/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:50:59 PM; REPORTS; DATE: 1/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:50:59 PM; REPORTS; DATE: 1/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:50:59 PM; REPORTS; DATE: 1/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:50:59 PM; REPORTS; DATE: 1/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:50:59 PM; REPORTS; DATE: 1/1/2021 12:50:59 PM;	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datells Svent Datells	2/8/2021 Access Granted	436174446	10
- 14. A.	Report Date: 3/2/2021 1:57:42PM Sestem Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Swant Details Event Details		4541747(H 410432578	11 12
On Stand 7.4 Access Granted by Resider CULENY: START DATE: 2/1/2/21 12:00:00 AM; BOD DATE: 3/2/2011 11:00:09 PM; (RADESS: SNAT-1.2:00-PICK UP RM N), UACY1-2-2-2-4-BERMITOW ENT	Report Date: 3/2/2025 1:57:42PM Eastern Standard Time	Event Details	2/8/2021 Access Granted	4861,74753 4361,74753	39 14
Configured 7.4 Access Greeted by Preoder QUERY's SYART DATE: 1/1/2003 12:00:00 AM; But DATE: 1/1/2003 11:55:559 PM; BRADIRES: DAYA-1_2.00-9/CKU PF RM IN, DAY-1_2.00-9/CKU PF RM IN, DA	Report Date: 3/2/2021 1:57/42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Octob	2/8/2021 Access Granted	241924336	15 16
TARTO DE PLANTA DE PROPERTA DE	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detell Event Detell	s 2/8/2021 Access Grented	341912931 341912931	17
On Grant of A. Access Granted by Reader QUERY: START DATE: 2/1/2001 13:00:00 Abs. EMPLOATE: 3/1/2001 13:00 Abs. EMPLOATE: 3/1/	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detell		341975372 341926637	7a 78
DoScard 7.4 Access Grenhed by Reader QUERY: START DATE: 2/1/2011 12:0800 AM; BND DATE: 3/1/2011 11:5939 PM; READERS: DAVI-12:05-PKS: UP ARR IV, UNIVI-12-CO-04-BEARMOON BNT DATE: 3/1/2011 11:5939 PM; READERS: DAVI-12:05-PKS: UP ARR IV, UNIVI-12-CO-04-BEARMOON BNT DATE: 3/1/2011 11:5939 PM; READERS: DAVI-12:05-PKS: UP ARR IV, UNIVI-12-CO-04-BEARMOON BNT DATE: 3/1/2011 11:5939 PM; READERS: DAVI-12:05-PKS: UP ARR IV, UNIVI-12-CO-04-BEARMOON BNT DATE: 3/1/2011 11:5939 PM; READERS: DAVI-12:05-PKS: UP ARR IV, UNIVI-12-CO-04-BEARMOON BNT DATE: 3/1/2011 11:5939 PM; READERS: DAVI-12:05-PKS: UP ARR IV, UNIVI-12-CO-04-BEARMOON BNT DATE: 3/1/2011 11:5939 PM; READERS: DAVI-12:05-PKS: UP ARR IV, UNIVI-12-CO-04-BEARMOON BNT DATE: 3/1/2011 11:5939 PM; READERS: DAVI-12:05-PKS: UP ARR IV, UNIVI-12-CO-04-BEARMOON BNT DATE: 3/1/2011 11:5939 PM; READERS: DAVI-12:05-PKS: UP ARR IV, UNIVI-12-CO-04-BEARMOON BNT DATE: 3/1/2011 11:5939 PM; READERS: DAVI-12:05-PKS: UP ARR IV, UNIVI-12-CO-04-BEARMOON BNT DATE: 3/1/2011 11:5939 PM; READERS: DAVI-12:05-PKS: UP ARR IV, UNIVI-12-CO-04-BEARMOON BNT DATE: 3/1/2011 11:5939 PM; READERS: DAVI-12:05-PKS: UP ARR IV, UNIVI-12-CO-04-BEARMOON BNT DATE: 3/1/2011 11:5939 PM; READERS: DAVI-12:05-PKS: UP ARR IV, UNIVI-12-CO-04-BEARMOON BNT DATE: 3/1/2011 11:5939 PM; READERS: DAVI-12:05-PKS: UP ARR IV, UNIVI-12-CO-04-BEARMOON BNT DATE: 3/1/2011 11:5939 PM; READERS: DAVI-12:05-PKS: UP ARR IV, UNIVI-12-CO-04-BEARMOON BNT DATE: 3/1/2011 11:5939 PM; READERS: DAVI-12:05-PKS: UP ARR IV, UNIVI-12-CO-04-BEARMOON BNT DATE: 3/1/2011 11:5939 PM; READERS: DAVI-12:05-PKS: UP ARR IV, UNIVI-12-CO-04-BEARMOON BNT DATE: 3/1/2011 11:5939 PM; READERS: DAVI-12:05-PKS: UP ARR IV, UNIVI-12-CO-04-BEARMOON BNT DATE: 3/1/2011 11:5939 PM; READERS: DAVI-12:05-PKS: UP ARR IV, UNIVI-12-CO-04-BEARMOON BNT DATE: 3/1/2011 11:5939 PM; PM ARR IV, UNIVI-12-CO-04-BEARMOON BNT DATE: 3/1/2011 11:5939 PM; PM ARR IV, UNIVI-12-CO-04-BEARMOON BNT DATE: 3/1/2011 11:5939 PM; PM ARR IV, UNIV-12-CO-04-BEARMOON BNT DATE: 3/1/2011 11:5939 PM; PM ARR IV, U	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detail	a 2/8/20121 Azznes Grented	241889908 241912682	2D 21
OmSuard 7.4 Access Greened by Render CLERY: START DATE: 2/1/2021 12:00:00 ANA; END DATE: 3/2/2021 12:95:59 PM; RENDER: DAY; 2 O DATE: 2/2/2021 12:90:00 PM III, DATE: 2/2/2021 12:95:50 PM; RENDER: DAY; 2 O DATE: 2/2/2021 12:90:00 PM III, DATE: 2/2/2021 12	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detail Event Oatsi	s 2/6/2021 Access Grented	241591099	22
On Suard 7.4 Access Genetad by Reader OLERY: START DATE: 2/1/2013 220000 ANI; SHD DATE: 1/2/2013 125559 PM: READERS: OAX-1_2.03-PEX.UP RM IN, DAY-1_2.03-REAGROOM EN	Report Date: 3/2/2021 1:57:43PM Eastern Standard Time Report Date: 3/2/2023 1:57:42PM Eastern Standard Time	Event Detel			23 24
On Sun 17.4 Access Granted by Residen ULERY: START DATE: \$\frac{1}{2}\int 2000 OAM; BID DATE: \$\frac{1}{2}\int 2001 DATE: \$\frac{1}{2}\int 2001 T1: \$\frac{1}\int 2001 T1: \$\frac{1}{2}\int 2001 T1: \$\frac{1}\int 2001 T1: \$\frac	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detail	ls 2/8/2021 Access Granted		25 26
Christanti 7.4 Access Standard by Render CUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:50:59 PM; READERS: CART-1.203-PICE UP RM IN, DAY-1-2.104 REPARRISON ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detail	s 2/8/2021 Access Greenter	341919644	27
On General 7.4 Accessed General by Resident QUERTY START DATE: 2/1/2021 12:00:000 AMI, DIAD THE 3/1/2021 11:30:000 FM, HOUSE THE START DATE: 2/1/2021 11:30:	Report Date: 3/2/2021 1:5742PM Eastern Standard Time Report Date: 3/2/2021 1:5742PM Eastern Standard Time	Event Detail		241925218 341891579	28 29
OnGuard 7.4 Acoust Granted by Reader QUERY: START DATE: 2/1/2021 12:0000 AM; BHD DATE: 2/1/2021 11:5559 PM; READERS: CAXY-1-2:0-FKX UP RM HI, DAXY-1-2:0-FKX UP RM HI, DAXY	Report Date: 3/2/2021 1:57:429ld Eintern Standard Time	Event Date	ls 2/8/2021 Access Grantes		30 31
	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Debi	la 2/8/2021 Access Grantin	486178476	32
Ordeard 7.4 Access General by Resider CHORACT STATE DATE: STATE DATE: \$21/2011 120000 JAM; BID DATE: \$1/2011 115509 PM; RIGHTS; DATE: 1.2009 PCI PM INI, DATE: 1.2009 RET CHORACT 7.4 Access General by Resider CHORACT 7.4 Access General by Resider CHORACT 7.4 Access General by Resider CHORACT 7.5 Access General by Resider CH	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Cam			93 34
On Guard 7.4 Access Granted by Reader QUERY: \$7487 DATE: \$7/4201 12/0000 ANY; BIO DATE: \$7/4201 1150659 PM; READERS: \$0XY-1.2.0.0 PM; RUGUEL UR MIN, DOX-1.2.0-0-REARROOM RET	Report Date: 3/2/2021 1:57:42FM Sestem Standard Titres	Event Deta	fis 2/8/2021 Access Grante	341924402	25 36
On Suand 7.4 Access Granted by Render CKLERY: START DATE: 2/1/2021 12:00:00 AM; SND DATE: 3/2/2021 11:59:59 PM; READERS: DAXY-1-2.09-90X-90X-90X-90X-90X-90X-90X-90X-90X-9	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 9/2/2021 1:57:42PM Eastern Standard Time	Event Deta Event Deta	ffs. 2/8/2021 Access Grante	251303698	37
On Bland 7/A Acres Granted by Reader OLIENT STATE DATE: 1/4/2011 11:00:00 AMI, 190 DATE: 1/4/2011 11:00 DATE: 1/4/2011 11:00:00 AMI, 190 DATE: 1/4/2011 11:00 DATE: 1/4/2011 11:00:00 AMI, 190 DATE: 1/4/2011 11:00:00	Report Cate: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Dete	lls 2/9/2021 Access Brante		38 39
OnSurrd 7.4 Access Graphed by Reader OMENT: STATE DATE: 2/3/2021 12:50:50 AND: 2/3/2021 12:50:50 PM (READERS: DAXY-12:GR-PICKUP WHIN, DAXY-12:OH-BEAUGUOUN BY) OMENT: STATE DATE: 2/3/2021 12:50:50 PM (READERS: DAXY-12:GR-PICKUP WHIN, DAXY-12:OH-BEAUGUOUN BY)	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Oeta	No 2/6/2021 Access Grente	341834554	40
	Support Outs: 9/2/2021 1:57:42PM Eastern Standard Time Raport Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Deta	ils 2/8/2021 Access Grante	d 3419196Z2	41 42
OnBusid 7.4 Access Granhed by Reader QUSRY: START DATE: 2/3/2011 12:00:00 AME: 1/7/2011 13:9559 PM; READERS: DAVY-12.00-PULL UP ROLIN, DAV-12-00-READERS DAVY-12.00-PULL UP ROLIN, DAV-12-00-READERS DAVY-12.00-PULL UP ROLIN, DAV-12-00-READERS DAVY-12.00-PULL UP ROLIN, DAV-12-00-READERS DAVY-12.00-PULL UP ROLIN, DAV-12-00-PULL UP ROLING DAV-12-00-	Report Date: 3/2/2021 1:57:42PM Sectors Standard Time	Event Deu	ills 2/8/2021 Access Grants	d 341891665	43 44
On Guard 7.A Access Stantad by Reader QUERY: START DATE: 2/1/2011 12:00001 ANI; BYD DATE: 3/1/2011 13:0009 PROSESS Fall READERS: DATE: ACCULATION FOR DATE: 2/1/2011 13:0000 PROSESS Fall READERS: DATE: ACCULATION FOR DAT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Eyent Dub	ills 2/8/2021. Access Grente	d 289219546	45 46
THE PARTY NAME OF STREET PARTY	Report Date: 3/2/2021 1:57:62PM Sentem Standard Tim Report Date: 3/2/2021 1:57:62PM Featern Standard Tim	e Event Det	alls 2/8/2021 Access Grents	d 241690101	47
On Suard T.A. Recome Graphed by Reader CLERY: STANT DATE: 2/1/2/01 12/08/00 AM; BID DATE: 3/2/2/01 11/08/09 PM; READERS: OAXY-1,2.00-PM; UP NM IN, DATY-1,2.00-deceasabless of the Company of the Compan	Report Date: 8/2/2021 5:57:12PM Eastern Standard Tim	e Event Det	ells 2/8/2021 Access Grants	d 2513/6696	48 49
On Quart 7.4 Access Granted by Reader QUERY: START DATE: 2/1/2001 12:00:00 AAR; END DATE: 2/1/2002 11:05:05 PM; READERS: DATE-1.1.0.FPICK UP RMIN ROATE-1.2.0-48EARCOM ENT	Report Date: 3/2/2021 1:57:42PM Eintern Standard Tim Report Date: 3/2/2021 1:57:42PM Eastern Standard Tim	e Event Det	ells 2/5/2021 Access Grants	d 289219546	50
OnGuerd 7.4 Access Granted by Reader OUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXY-1.2/02-PICK UP RENTW, DAXY-1.2:00-PICK UP RENTW, DAXY-1.2:0	Report Date: 3/2/2021 1:57:42PM Eastern Standard Tim Report Date: 3/2/2021 1:57:42PM Eastern Standard Tim	 Event Dat 		rd 341854594	51 52
OnStart 7.4 Access Granted by Reader OLEXY: START CATE: 3/1/2021 1200000 ANI; END DATE: 3/2/2021 1195559 PM; READERS: DAZI-1_203-PULC UP RIVIN, DAZI-1_2-0-REAXROOM ENT	Report Date: 3/2/2021 157:42FM Eastern Standard Tim Report Date: 3/2/2021 1:57:42FM Eastern Standard Tim	e Event Det	alls 2/8/2021 Access Grent	ed 341508144	59
On Bund 7.A. Access Granted by Reader ONEW; STACT DATE: \$1/1/201 12:00:00 AM; Biol DATE: \$1/2/201 11:59:59 PM; READING: DATE: 12:00-PMX UP BM IN, DAY: 12:00-PMX UP BM IN,	Report Date: 3/2/2021 1:57:429M Eastern Standard Tim	as Event Det	mils 2/8/2021 Ancess Grant	ed 458174753	22
Cindiamo 7.4 Access Granted by Resider OURRY: START DATE: 2/1/2021 12:00:00 0AN; END DATE: 3/1/2021 11:50:50 PM; READERS: DADD-1.2.03-PMX UP RM M, DAX2-1.2.04-REARROOM ENT CINDIAMO ACCESS Granted by Reader OURRY: START DATE: 2/1/2021 12:00:00 0AM; END DATE: 3/1/2021 11:50:50 PM; READERS: DADD-1.2.03-PMX UP RM M, DAX2-1.2.04-REARROOM ENT	Report Date: 2/2/2021 1:57:42PM Eastern Standard Tim	e Event Det	injls 2/II/2021 Access Grant	41.0452873	56

OnGuera 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM;	END DATE: 3/2/2021 11:59:58 PM	READERS: DAXY-1,2.08-PICK UP RM IN, DAXY-1,2.04-BREAKROOM ENT	Seport Oute: 3/2/2021 1:57:42PM Extern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Optails	2/8/2021 Access Granted 2/8/2021 Access Granted	416192950 252616484	57 58
OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Granted by Reader	MINERAL ATTEMPT THATES, 125 (1974) 12-09-03 \$32-	EMD DATE: 3/2/2071 11:59:59 PM:	READERS: DAX7-1.2,03-PICK UP RIM IN, DAX7-1.2,04-BREAXROOM ENT READERS: DAX7-1.2,03-PICK UP RIM IN, DAX7-1.2,04-BREAXROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datells Event Datells	2/8/2021. Access Granted 2/8/2021. Access Granted	434174759 434192950	50 50
On Stand 7.4 Access Granted by Reeder	PARTY. CTART DATE: 2/1/2001 12:00:00 AM:	END DATE: 1/2/2021 11:59:59 PM:	READERS: DAXT-1.1.03-PICK UP RM IN, DAXT-1.1.04-89EAKROOM ENT READERS: DAXT-1.1.06-PICK UP RM IN, DAXT-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Sestem Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detells	2/8/2021 Access Granted	257170633	6).
OnGuard 7.4 Access Granted by Reader UnGuard 7.4 Access Granted by Reader	OLDERY: START DATE: 2/1/2021 12:00:00 AM:	END DATE: 3/2/2021 11:59:59 PM	READERS: DAX7-1.2.09-PICK UP RM IN, DAX7-1.2.04-BILEAKKUCIM EN I	Report Date: 3/2/2021 1:57/42PM Earners Standard Time Report Date: 3/2/2021 1:57/42PM Eastern Standard Time	Event Details	2/8/2021 Access Granted 2/8/2023 Access Granted	134691775 269216791	6
OnGuard 7.4 Access Granted by Reeder	OLDERY, 67ART DATE: 3/1/2023 12:00:00 AM:	AND DATE: 3/2/2021 11:59:59 PM	READERS: DAKY-1.2.03-PICK UP RIM IN, DACY-1.2.04-BREAKROOM ENT READERS: DAKY-1.2.03-PICK UP RIM IN, DAKY-1.2.04-BREAUGOOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datells	2/6/2021 Access Sminted 2/8/1021 Access Branted	341906822 251334730	64 65
OnGuard 7.4 Access Granted by Reeder OnGuard 7.4 Access Granted by Reeder	CHECKS, PRANTINGS 1/2/2011 13-0000 AM-	FND DATE: 3/3/2071 11:59:59 PM	READERS: DAX7-1.2.03-PKX UP RM IN, DAX7-1.2.04-INVARIANCEM EN	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Cotalia	2/8/2021 Access Granted	343918864	66
OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Granted by Reader	ALTERNATION OF PRINCIPLE STATES AND ADDRESS AND ADDRES	- IDMS PATE: 3/2/2021 11/59/59 PM	READERS: DAX7-1.2.09-PICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT ; READERS: DAX7-1.2.09-PICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57/43PAA Eastern Standard Time Report Date: 3/2/2021 1:57/43PAA Eastern Standard Time	Event Details	2/8/2021 Acress Granted 2/8/2021 Acress Granted	261672745 218611830	67 68
OnGuerd 7.4 Access Granted by Ressist	QUERY: START DATE: 2/1/2021 12:00:00 AM	: BND DATE: 3/2/2021 11:59:59 PM - BND DATE: 3/2/2021 11:59:59 PM	READERS: DAX7-1.2.03-PICK OF SMITH, DAX7-1.2.04-BREACHOOM ENT	Heport Date: 3/2/2021 1:55:429th Eastern Standard Vince	Event Datalis	2/6/2021 Alcess Granies	341225921 251334881	69 70
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On Guard 7.4 Access Granted by Reader On Guard 7.4 Access Granted by Reader			CHARGERS: DAXT-1.2.09-PIEX UP RM IN, DAXT-1.2.04-BREAGROOM ENT CHARGERS: DAXT-1.2.09-PIEX UP RM IN, DAXT-1.2.04-BREAGROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/8/2021 Access Granted 2/8/2021 Access Granted	410429943 341891027	72 78
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OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Granted by Reader	ALENA, PERFET DATE: 2/1/2011 13:00:00 AM	 FND DATE: 3/2/2021 11:59:59 PM 	I: READERS: DAX7-1,2.05-PICK UP RM IN, DAX7-1,2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detells Event Details	2/9/2021 Access Granted 2/9/2021 Access Granted	436177789	2
OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Granted by Reader	ALTERNATION TO A PROPERTY AND ADDRESS AND	 But NATE: 3/2/2001 11:59:58 PM 	C READERS: DAXT-1.2.08-PICK UP RM IN, DAXT-1.2.04-BREADROOM ENT IS READERS: DAXT-1.2.03-PICK UP RIM IN, DAXT-1.2.04-BREADROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Catella Event Datella	2/9/2021. Access Granted 2/9/2021. Access Granted	341921803 410433578	3 4
OnStard 7.4 Access Granted by Reader	QUERY: START CATE: 2/1/2021 12:00:00 AM	; END DATE: 8/2/2021 11:59:59 PN	I; READERS; DACK-1.2.03-PICK OF RM IN, DACK-1.2.04-BREADERS ON ENT	Report Date: 3/2/2021 1:57:43PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datalis	2/9/2021 Access Granted	241890454 341890288	5
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OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM	■ GMD OMTE - 1/7/2071 11:59:59 PK	R: READERS: DAXY-1.2.03-PIOK UP RM IN, DAXY-1.2.04-SREALROOM ENT A: READERS: DAXY-1.2.03-PIOK UP RM IN, DAXY-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/9/2021 Access Granted	436174445 341892062	10 11
OnGuerol 7.4 Access Granted by Render OnGuard 7.4 Access Granted by Render	ALERSON PROPERTY 2/1/2021 12:00:00 Mile	 PMD DATE: 3/3/2021 11:59:59 PM 	A: READERS: DAXY-1, 2:08-PICK UP HM IN, GUGY-1, 2:04-SIZEM/SULM: ENI	Report Date: 3/2/2021 1:57:42PM Feature Standard Time Report Date: 3/2/2021 1:57:42PM Feature Standard Time	Event Details Event Details	2/9/2021 Access Granted 2/9/2021 Access Granted	341906470	12
Onderent 7.4 Access Granted by Reader On Guard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AN	 PMD DATE: 3/2/2021 11:59:59 Pf 	A; READERS: DAY7-1-2.03-PICK UP RIM IN, DAY7-1-2.04-RREAKROOM ENT A; READERS: DAX7-1-2.03-PICK UP RIM IN, DAX7-1-2.04-BREAKROOM ENT	Report State: 3/2/2021 1:57:429M Excess Standard Time	Event Datells Event Octalls	7/9/2021 Access Granted 2/9/2021 Access Granted	341890239	13 14
OnGuard 7.4 Access Granted by Reads	QUERY: START DATE: 2/1/2021 12:00:00 AA	1; END DATE: 3/2/2021 11:59:59 P1	A; READERS; DAKT-1.2.03-PICK UP RM IN, DAKT-1.2.04-BREAKROOM ENT A: READERS; DAKT-1.2.03-PICK UP RM IN, DAKT-1.2.04-BREAKROOM ENT	Report Date: 3/2/2023 1:57:42PM Eastern Standard Time Report Date: 9/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/9/2021. Access Granted	341922804 341924336	15 16
OnGuerd 7.4 Access Branted by Reade OnGuerd 7.4 Access Branted by Reade	ALL HARMS AND ADDRESS OF A SERVICE ASSESSMENT ASSESSMEN	 Dars Darrie, 3/2/2021 11/53/58 Pf 	4 - BEADERS: DAXY-1.2 CH-PICK UP RM IN, DAXY-1.2.OI-BREASHODAS ENT	Report Date: 3/2/2021 157:42PM Enstern Standard Time Report Date: 3/2/2021 157:42PM Eastern Standard Time	Event Details Event Details	2/9/2021. Access Granted 2/9/2021. Access Granted	341925627	17
OnStand 7.4 Access Granted by Reade DriGuard 7.4 Access Granted by Reads	QUERY: START DATE: 2/1/2021 12:00:00 AA	 END DATE: 3/2/2021 11:59:59 P! 	M: READERS: DAX7-1.2.03-PICK UP RIM (N, DAX7-1.2.04-BREAKROOM ENT M: READERS: DAX7-1.2.03-PICK UP RIM IN, DAX7-1.2.04-BREAKROOM ENT	Report Oate: 3/2/2021 1:57:42PM Eastern Standard Time:	Event Oetalls Event Details	2/9/2021 Access Granted 2/9/2021 Access Granted	341890A94 341920107	18 19
OrrGuard 7.A Access Stanted by Reads	DUERY: START DATE: 2/1/2021 12:00:03 AA	 EUR DATE: 3/2/2021 11:59:59 PI 	W; READERS: CANO-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT W; READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:62PM Eastern Standard Tiret Report Date: 3/2/2021 1:57:12PM Eastern Standard Time	Event Details	2/9/2021 Agonya Granted	341920062	20
On Guard 7.4 Access Granted by Reads On Guard 7.4 Access Granted by Reads	ALCOHOL: PERSON NAME: 1/4 (1973) 12:00:00 55	a. Even marcii 3/2/2021 11/59/50 FI	UF READERS: DAX7-12.08-PICK UP HM IN, DICLF-12.09-BHDURG GM ENI	Report Date: 9/2/2021 157:42PM Eastern Standard Time Suport Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detrills	2/9/2021 Access Granted 2/9/2021 Access Granted	341824226 341924402	21
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OnGuard 7.4 Access Granted by Reads OnGuard 7.4 Access Granted by Reads	OLIERY: START DATE: 2/1/2011 12:00:00 A	A FND DATE: 3/2/2021 11:59:59 P	M; READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREADROOM ENT M; READERS: DAX7-5.2.03-PICK UP RM IN, CAR7-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Datells Event Datells	2/9/3021 Access Grantesi	436174401	25
OnGuard 7.4 Access Granted by Reads OnGuard 7.4 Access Granted by Reads	- ALMERY, WART DATE: 10/2021 12:00:0041	4. END DATE: 3/2/2023 23:59:58 Pt	M: READERS: DAX7-1,2,08-PICK UP RM IN, DAX7-1,2,04-BYEAGECUM ENV	Apport Date: 3/2/2021 1:57/42PM Eastern Standard Time Report Date: 3/2/2021 1:57/42PM Eastern Standard Time	Event Details	2/9/2021 Access Granted 2/9/2021 Access Granted	43 61.78476 841892767	26 27
On Suand 7.4 Access Granted by Reads	QUERY: START DATE: 2/1/2021 12:00:00 A	W; END DATE: 3/2/2021 1159:59 P W: FIND DATE: 3/2/2021 1159:59 P	M; READERS: DAK7-1,2.03-PKK UP RM IN, DAX7-1,2.04-BILDAKKUUM ENT M: READERS; DAU7-1,2.03-PKK UP RM IN, DAX7-1,2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Svent Details	2/9/2021. Access Granted 2/9/2021. Access Granted	289215492 410433666	28. 29
OnGuard 7.4 Access Granted by Reads OnGuerd 7.4 Access Granted by Reads	 QUERY: START DATE: 2/1/2021 12:00:00 A 	Ver PMD DATE: 18/2/2021 11:59:59 P	M; READERS: DAX7-12-05-PICK UP RM IN, DAX7-12-01-BREASRO DIM ENT M; READERS: DAX7-12-05-PICK UP RUN IN, DAX7-12-04-BREASROOM ENT	Report Date: 3/2/2021 1.57:42PM Eastern Standard Time Report Date: 1/2/2021 1.57:42PM Eastern Standard Time	Event Details Event Details	2/9/2021 Access Branted	436174665	30
OnGuard 7.4 Access Granted by Reed OnSuard 7.4 Access Granted by Read	CUERY: START DATE: 2/1/2021 12:00:00 A	M: END DATE: 1/2/2021 11:59:59 P	M: READERS: DAX7-1.2.09-PICK UP IIM IN, DAX7-1.2.09-BIBADICIUM BY)	Report Oute: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 2/2/2021 1:57:42PM Eastern Standard Time	Event Detalla Event Detalla	2/9/2021 Access Grented 2/9/2021 Access Grented	341891533 841919622	31 22
OnStand 7.4 Access Granted by Read	H QUERY: START DATE: 2/1/2021 12:00:00 A	M; END DATE: 3/2/2021 11:59:89 P	M; READERS: DAILY-1,2,03-PICK UP HAN IN, DAXY-1,2,04-BHEARNOOM ENT No. READERS: DAXY-1,2,03-PICK UP RM IN, DAXY-1,2,04-BREARROOM ENT	Report Data: 3/2/2021 1:57:42PM Eastern Standard Time	Swent Datalis	2/9/2021 Access Granted	341892084 341922970	33 34
OnGuard 7.4 Access Granted by Reed OnGuard 7.4 Access Granted by Reed		NA - DNO DATE : 9/2/2021 11:59:50 P	M; READERS: DAXT-1.2.09-PICK UP RM1N, DAXT-1.2.04-BREAKROOM ENT M; READERS: DAXT-1.2.08-PICK UP RM1N, DAXT-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021, 1,57/42PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details	2/9/2021 Access Granted 2/9/2021 Access Granted	341890520	35
On Swind 7.4 Access Branted by Read On Buard 7.4 Access Granted by Raud	CULTURE START DATE: 3/1/2011 12:00:00 A	M: FNII DATE: 3/2/2021 11:58:89 P	WA: READERS: DAXY-1,2112-PICK UP RM IN, DAXY-1,204-BREAKNOWN ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/9/2021 Access Granted 2/9/2021 Access Granted	341924513 3418£3757	26 27
OpGuard 7.4 Access Grented by Read OnGuard 7.4 Access Grented by Read	QUERY: START DATE: 2/1/2021 12:00:00 A	M; END DATE: 3/2/2021 11:59:59 9 M: PND DATE: 9/2/2021 11:59:59 9	Mr. READERS: DAXY-1.2.03-PICK OF HIS IN, DAXY-1.2.04-PRESERVOOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Three	Event Datalls	2/9/2021 Access Granted 2/9/2021 Access Granted	436174379 341510988	38 39
OnGuard 7.A. Access Stanted by Rand		M+ END DATE: 1/2/2021 11:59:59 F	"M; READERS; DAXT-1.2.03-PICK UP RM (N, DAXT-1.2.04-BREAKROOM ENT "M; READERS; DAXT-1.2.03-PICK UP RM (N, DAXT-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/9/2021 Access Granted	341691721	40
On Buard 7.4 Access Granted by Read On Guerré 7.4 Access Granted by Read	ALMENY, START DATE: 351/3001 13:00:00 8	AA- END DATE: N/2/2021 11:59:59 F	ME: READERS: DAXX-1.2.03-PICK UP RM INCOMX-1.2.00-SIR2-BURGLEM ENT	Report Date: 3/2/2021 1:57:12FM Septem Standard Time Report Date: 2/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/9/2021 Access Granted 2/9/2021 Access Granted	496192950 215838646	42
OnGuard 7.A Access Granted by Read	or QUERY: START DATE: 2/1/2021 12:00:00 A	M; END DATE: 3/2/2021 11:59:56 M; END DATE: 3/2/2021 11:59:56	PM; READERS: DAX7-1.2,03-PICK UP RM IN, DAX7-1.2,01-BREAKROOM EN PM; READERS: DAX7-1.2,03-PICK UP RM IN, DAX7-1,2,01-BREAKROOM EN	Report Date: 3/2/2021 1:57:42PM Eastern Standard Tires	Buncht Dutally	2/9/2021 Access Granted 2/9/2021 Access Stanted	341520239	45 44
On Guard 7.4 Access Granted by Read On Guard 7.4 Access Granted by Read	W QUERY: START DATE: 2/1/2021 12:00:00 A	MA- END DATE: 1/2/2021 11:58:59	PM; READERS: DAYT-1.2.08-PICK UP RM IN, DAYT-1.2.04-BREARROOM ENT PM; READERS: DAYG-1.2.03-PICK UP RM IN, DAYT-1.2.04-BREARROOM ENT	Report Date: 3/1/2021 1:57:42PM Eastern Standard Wint Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/9/2021. Access Granted	341924094	45
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OnGuard 7.4 Access Granted by Read OnGuard 7.4 Access Granted by Read	Inr QUERY: START DATE: 2/1/2021 12:00:00)			Report Date: 11/2/2021 1:57:42PM Fastern Standard Time	Event Details	2/9/2021 Access Granted 2/9/2021 Access Granted	34)891781	20
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DeGuard 7.4 Acress Granted by Res	III QUERY: START DATE: 2/1/2011 12:00:00:	MM; END DATE: 3/2/2021 11:59:59 MM; END DATE: 2/2/2021 11:59:59	PM; READERS; DAXY-1,2.03-PICK UP RM IN, DAXY-1,2.04-BREAKRO DM EN	Report Date: 3/2/2021 1:57:42PM Sastum Standard Tim	Event Detalls	2/9/2021 Aconsi Granted	4216192930 1214891775	54
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On Suend 7,4 Acress Grented by Rea	L. OURSE, PRANCIONES, 271/2021 12:00:00	AM - FAM PACE: 8/2/2021 11:59:59	PM; READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREAKROOM EP PM; READERS: DAX7-1.2.08-PICK UP RM IN, DAX7-1.2.04-BREAKROOM EF	Report Date: 3/2/2021 1:57>42PM Entern Standard Tim	a Event Detalls	1/9/2021 Access Granted 2/9/2021 Access Granted	48.6180705 341936738	66 64
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OnGused 7.A Access Granted by Re-	der QUERY: START DATE: 2/1/2021 12:00:00	ANG. OND DATE: 3/3/2023 11/58/50	PM; READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREAKROOM EI PM; READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREAKROOM EI	Report Date: 3/2/2021 1:57:42PM Eastern Standard To	e Event Dateld	7/10/2021 Access Granted	436174753	3
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DoSuard 7.4 Access Granted by Re	pder QUERY: START DATE: 2/1/2021 17:00:00	ANA- FIND DATE: 2/2/2021 11:50:5/	PRO; READERS: DAXY-1.2.03-PICK UP RIM IN, DAXY-1.2.04-BRSAKROOM B PRO; READERS: DAXY-1.2.03-PICK UP RIM IN, DAXY-1.2.04-BRSAKROOM B	T Report Date: 3/2/1021 1:57:42PM Eastern Standard Tir	ne Event Debels	2/10/2021 Access Granted 2/10/2021 Access Granted	941919556 241890454	5 7
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Drieward 7.4 Access Granted by M	adar QUERY: SYART DATE: 2/1/2021 12:00:0	AM; END DATE: 1/2/2021 11:59:5	R PM; READERS: DAX7-1,2/III-PICK UP KIM IM, IMA7-1,2/OF-BREAKROOM E R PM; READERS: DAX7-1,2/OR-PICK UP KIM IM, DAX7-1,2/OF-BREAKROOM E	NT Report Date: 3/2/2021 1:57:42PM Eastern Standard To	ne - Event Debuli	2/10/2021 Access Granted	341913079	13
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On Guard 7.4 Access Granted by Re	uder QUERY: START DATE: 2/1/2011 12:00:0	O AM; END DATE: 1/2/2021 11:595 O AM; END DATE: 3/2/2021 11:595	S PM; READERS: DAX7-12/18-PICK OF RIGHT, DAX7-12/01-SREAKROOM I	NT Report Date: 3/2/2021 1:57:42PM Eastern Standard TI	me Evant Dete≨d	2/10/2021 Access Granted	436174401	18 19
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On Guard 7.4 Access Granted by 1 On Guard 7.4 Access Granted by 1	. I. ALEMA PROPERTY 1/1/2021 17:02	ON ANA - THEN DATE: 8/2/2021 11:59	SSIP PIME READERS: DAXO-1,2,03,PICX LIP RIM IN, DAXO-1,2,04-BREARROUM	ENT Report Date: 8/2/2021, 1:57:42PM Eastern Standard	Irse Event Detel		341891731	36 97
Unifound 7.4 Access Stranted by	lauder QUENT: START DATE: 2/1/2021 12:00	00 AM; END DATE: 3/2/2021 11:55	59 PM; READERS: DAXX-1.2.03-PICK UP RM IN, DAX7-1.2.04-PREADERS SER PM: DEEDERS: DAXX-1.2.08-PICK UP RM IN, DAX7-1.2.04-PREADERSOOM	ENT Apport Date: 3/2/2021 1:57:42PM Eastern Standard	Trne Event Debi	2/10/2021, Access Grants		98 20
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Candium of 7.4 Access Granted by	Render QUERY: START DATE: 2/1/2021 12:00	:00 AM; END DATE: 3/2/2021 11:56 :00 AM; END DATE: 3/2/2021 11:56	159 PM; READERS; DAGY-1, Z.08-PICK UP RIM IN, DAGY-1, 2,04-BREAKROON	ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard	firm Event Date	ls 2/10/2021 Access Grants	d 341906668	43 44
OnGuard 7.4 Access Granted by OnGuard 7.4 Access Granted by	Render QUERY: START DATE: 2/1/2021 12:00	HOD AM - END DATE: 3/3/2021 11:55	199 PM: READERS: DAX2-1,2.03-PICK UP RM IN, DAX7-1224-HARDON-000	FAIT Broom Date: 3/2/2021, 1:57:42PM Eastern Standard	Time Event Deta	le 2/10/2021 Access Grants	d 341691731	45
On Guard 7.4 Access Granted by On Guard 7.4 Access Granted by	- / NICON, CONTRACTO SALESCO 1200	ENGRAM: DNO DATE: 3/2/2021 11:59	rsa PM; Readers; Dad7-1.2.08-PICK UP RM (M, DAX7-1.2.04-BREAURO OH RSB PM; READERS; DAU7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREAURO OH	ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard	Time -Event Data			
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OnGuard 7.4 Access Granted by OnGuard 7.4 Access Granted by			2:59 PM; READERS: DAX?-1,2.03-PICK UP RM IN, DAX?-1,2.04-BREAKROON ILSB PM; READERS: DAXT-1,2.03-PICK UP RM IN, DAXT-1,2.04-BREAKROON	ENT Report Date: 3/2/2021 1:57 HZPM Exstern Standard	Time Event Det			
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OnGuard 7.4 Access Granted by OnGuard 7.4 Access Granted by	 A. CLEDV, PTART DATE: 2/1/2015 12/0 	1-00 AM - PND DAYE: 3/2/2021 11:5	9:59 PM: READERS: DAX2-1-2.118-PNLX UP RM IN, SAULY-1-2-0-PM-MANAGEMENT	SENT Report Date: 3/2/2021 1:57:42PM Eastern Standard	Time Event Dat	ille 2/10/2021. Access Grant	rd 410429965	5 55
OnGuerd 7.4 Access Branked by OnGuerd 7.4 Access Granted in	ALICON, CTART DATE: 1/1/2021 12:0	n nn ami: PMC DATE: 3/2/2021 11:5	9:55 FM; READEIS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREAKROON 9:58 PM; READEIS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREAKROON	ENT Report Date: 3/2/2023. 1:57:42PM Eastern Standard	Time Event Det			57
OnGuerd 7.4 Access Granted by	Reader CLUERY: START DATE: 2/1/2021 12:0	0:00 AM; END DATE: 3/2/2021 11:5	BIRD PIM; READERS: DAXY-1.2.03-PICK UP RIMIN, DAXY-1.2.04-BREAKROOD BISG PIM; READERS: DAXY-1.2.03-PICK UP RIMIN, DAXY-1.2.04-BREAKROOD	A ENT Report Oats: 3/2/2021 L:57:42PM Eastern Standers	Time Eyent Dat	alls 2/10/2021 Access Grant	ed 25167274	
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ORGANITY A ROUSE GRANTED BY READY QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:50:59 FM; READERS; DAXT-1.2.03-PICK UF RM RI, DAXT-1.2.04-BREARROOM ENT ORBIGATY A ACCESS GRANTED BY READERS; DAXT-1.2.04-BREARROOM ENT ORBIGATY A ACCESS GRANTED BY READERS; DAXT-1.2.04-BREARROOM ENT	Report Outs: 3/2/2021 1:57:42PM Eastern Standard Tire?	vent Details	2/10/2021 Access Granted 2/10/2021 Access Granted	292746517 341906568	62 63
On Orange of A Access Granted by Render QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXT-1.2.03-PICK UP RM IN, DAXT-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	went Details	2/10/2021 Access Grented	Z5261G4B4	64
Configured 2.4 Assess Company On Figure 2/1/2021 12:00:00 AM: EVD DATE: 1/2/2021 11:59:59 PM; READERS; DAX7-1.2.03-FICK UP RM IN, DAX7-1.2.04-BREADERS CONTINUED FOR THE PROPERTY OF THE PROPE	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	vent Details	2/10/2021. Access Stanted 2/10/2021. Access Granted	410430081	65 66
Original 7.6 Access Greated by Reader OUERY, START DATE: 2/1/2021 12:0000 AM; END DATE: 3/1/2021 11:59:09 FM; READERS: DAZ-1_1.09-10.1.0 FM III, DATE: 3/1/2021 11:59:09 FM; READERS: DAZ-1_1.09-10.1.0 FM III, DAZ-1_1.00-10.0 FM III, DAZ-1_1.0 FM III, DAZ-1_1_1.0 FM III, DAZ-1_1_1_1.0 FM	Report Date: 3/2/2021 1:57:42PM Eastern Standard Tree	iven Details	2/10/2021 Apress Granted	289215338 341890101	-
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CHERT: STAFT DATE: 2/1/2021 12:00:00 AM: BND CATE: 2/2/2021 11:59:59 PM: READERS: CAX7-1.2.09-PICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Getails	2/11/2021 Access Grented 2/11/2021 Access Grented	496193060 841893027	9
CriGuard Z.A. Access Grantest by Reader QUERY: START DATE: 2/1/2021 11:00:00 AM; EVD DATE: 3/1/2021 11:39:59 PM; READERS: DAV7-1.2.05-PKX UP RMIN, DAV7-1.2.06-PROMOTOM ENT	Apport Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details	2/11/2021 Access Granted	341892282	10 11
OriGuerd 7.4 Access Systemed by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:58 PM; READERS: DAXX-1.2:03-PICK UP RM IN, DAX7-1.2:04-REARRIGOM PMT		Event Detells Event Detells	2/11/2021 Access Granted 2/11/2021 Access Granted	341912991 341812582	12
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CnGuard 7.4 Access Granted by Sauder CLERY: 57.470 ARE: 2/3/2021 1230000 ANE: 100 DATE: 2/3/2021 113000 PM: REDDERS: DAXY-1.2.23-FACK UP RMIN, DAXY-1.2-0-8BEAKRDOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datails	2/11/2021 Access Granted	341913079	15 16
On Sunct 7.4 Access Stanted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:59:59 PM; READERS: DASY-12:05-PCK UP RW IN, DATE-12:04-PCK UP RW IN, D	Report Date: 3/2/2021 1:57:62PM Eastern Standard Time	Event Details Event Details	2/11/2021 Access Granted 2/11/2021 Access Granted	241913169	17
CHERY, START DATE: 271/2021 12:00:00 AM: BND DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2:09-PICE UP RIM IN, DAX7-1.2:09-BREARROOM ENT	Report Date: 3/2/2021 1:575GPM Eastern Standard Time	Event Details	2/11/2021 Access Branked 2/11/2021 Access Granked	436174401 341885757	18 19
OnSward 7.4 Access Grazed by Reader QUERY: START DATE: 2/1/2021 13:00:00 AM; EVD DATE: 3/1/2021 11:35:59 PM; READERS: DAXT-1.2:05-PICK UP RM IN, DAXT-1,2:04-REARROOM ENT ORGUNI 7.4 Access Granted by Reader QUERY: START DATE: 2/1/2021 13:00:00 AM; EVD DATE: 3/1/2021 13:00:00 PM; READERS: DAXT-1.2:03-PICK UP RM IN, DAXT-1,2:04-REARROOM ENT ORGUNI 7.4 Access Granted by Reader QUERY: START DATE: 2/1/2021 13:00:00 AM; EVD DATE: 3/1/2021 13:00:00 PM; READERS: DAXT-1.2:03-PICK UP RM IN, DAXT-1,2:04-REARROOM ENT	Report Oats: 3/2/2021 1:57:43PM Eastern Standard Time	Event Details	2/11/2021 Access Granted	341924084 436174731	20 21
DARLINED Z.A. Access filmeted by Reader QUESTY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 FM; READERS: DAX7-1.2.03-FEX UP RM IN, DAX7-1.2.03-REARROOM ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time Report Date: 2/2/2021, 1:57:42PM Eastern Standard Time	Event Detrills	2/11/2021 Access Grented 2/11/2021 Access Grented	410431992	22
COUNTY A READ GOVERNMENT OF THE START DATE: 2/1/2020 12:00:00 AM: PND DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2:08-PICK UP RM IN, DAX7-1.2:08-BREAKROOM EN1	Report Date: 4/2/2021 1.57/42PM Eastern Standard Time Report Date: 3/2/2021 1.57-42PM Eastern Standard Time	Event Details Event Details	2/11/2021 Access Granted 2/11/2021 Access Granted	341913431	23 24
Choqued 7.A Access Granded by Reader CULERY: START DATE: 1/1/2021 12:00:00 AM; END DATE: 3/1/2021 12:00:00 PM; READERS: DATF-1.2.08-90CX UP RM IN, DATF-1.2.08-90EAND DATE: 0.00 PM; READERS: DATF-1.2.08-90CX UP RM IN, DATF-1.2.08-90EAND DATE: 0.00 PM; READERS: DATF-1.2.08-90CX UP RM IN, DATF-1.2.08-90EAND DATE: 0.00 PM; READERS: DATF-1.2.08-90CX UP RM IN, DATF-1.2.08-90EAND DATE: 0.00 PM; READERS: DATF-1.2.08-90CX UP RM IN, DATF-1.2.08-90EAND DATE: 0.00 PM; READERS: DATF-1.2.08-90CX UP RM IN, DATF-1.2.08-90CX UP RM IN, DATF-1.2.08-90EAND DATE: 0.00 PM; READERS: DATF-1.2.08-90CX UP RM IN, DATF-1.2.08-90CX UP R	Report Date: 1/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/31/2021 Access Granted 2/31/2021 Access Granted	841003414 841003548	25 26
On Suit of A. Access Granted by Reader CILERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DATE-1.2.03-PICK UP RRA IN, DAX7-1.2.04-BILEAKBOOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:43PM Eastern Standard Time	Svent Details Event Details	2/11/2021 Access Branked	341925238	27
CALLEGE OF THE STATE OF THE STA	Report Date: 3/2/2021 1:57:42PM Sestem Standard Time Report Date: 2/2/2021 1:57:42PM Sestem Standard Time	Event Details Event Details	2/11/2023, Anoma Granted 2/11/2023, Access Granted	410432679 289219546	28 29
OreGuerd 7.4. Assess Gerheid by Raseler QUERY, START DATE: 2/1/2001 12/2002 AM: END DATE: 3/2/2001 115/559 PM; READERS: DAYA-12.03-PICK UP MININ, DAYA-12.00-PERCARGOM ENT CHERTY START DATE: 2/1/2001 12/2002 AM: END DATE: 3/2/2001 115/559 PM; READERS: DAYA-12.03-PICK UP MININ, DAYA-12.04-PERCARGOM ENT CHERTY START DATE: 2/1/2001 12/2002 AM: END DATE: 3/2/2001 115/559 PM; READERS: DAYA-12.03-PICK UP MININ, DAYA-12.04-PERCARGOM ENT CHERTY START DATE: 2/1/2001 12/2002 AM: END DATE: 3/2/2001 115/559 PM; READERS: DAYA-12.03-PICK UP MININ, DAYA-12.04-PERCARGOM ENT CHERTY START DATE: 2/1/2001 12/2002 AM: END DATE: 3/2/2001 115/559 PM; READERS: DAYA-12.03-PICK UP MININ, DAYA-12.04-PERCARGOM ENT CHERTY START DATE: 2/1/2001 12/2002 AM: END DATE: 3/2/2001 115/559 PM; READERS: DAYA-12.03-PICK UP MININ, DAYA-12.04-PERCARGOM ENT CHERTY START DATE: 2/1/2001 12/2002 AM: END DATE: 3/2/2001 115/559 PM; READERS: DAYA-12.03-PICK UP MININ, DAYA-12.04-PERCARGOM ENT CHERTY START DATE: 2/1/2001 12/2002 AM: END DATE: 3/2/2001 115/559 PM; READERS: DAYA-12.03-PICK UP MININ, DAYA-12.04-PERCARGOM ENT CHERTY START DATE: 2/1/2001 12/2002 AM: END DATE: 3/2/2001 115/559 PM; READERS: DAYA-12.03-PICK UP MININ, DAYA-12.04-PERCARGOM ENT CHERTY START DATE: 2/1/2001 12/2002 AM: END DATE: 3/2/2001 115/559 PM; READERS: DAYA-12.03-PICK UP MININ, DAYA-12.04-PICK UP MINING UP MI	Report Cata: 3/2/2021 2:57:42PM Seaters Standard Time	Event Details	2/11/2021 Access Granted	341922970 341892767	80 31
OnBoard 7.4 Access 6 rented by Reader QUERT: STAIT CATE: 2/1/2021 12:00:00 AH; END DATE: 3/2/2021 11:50:59 PM; READERS: DAXY-1.2:00-PICK UP RAN IN, DAXY-1.2:00-FICK UP RA	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/11/2021. Access Grented 2/11/2021. Access Granted	341890520	32
THE STATE OF THE S	Report Date: 9/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/11/2021 Access Granted 2/11/2021 Access Granted	341925350 251909052	33
On Sear of 7.4 Access Generated by Resider ON Sear of 7.4 Access	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datelle	2/11/2021 Access Granted	341882326	35 26
Ondered 7.4 Access Granted by Reader CALERT: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/2/2021 11:59:59 PM; READERS: DATE-1.2.03-PKK UP RM [N, DATE-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:62PM Eastern Stendard Time Report Date: 3/2/2021 1:57:62PM Eastern Stendard Time	Event Details	2/11/2021 Access Granted 2/11/2021 Access Granted	34 1889220 410433578	27
ORIGINAT 7.4 Access Granted by Reider ORIGINAT 7.4 Access Granted by Reider OURS: \$1/2001 12:0000 AN1; DATE: \$1/2001 12:0000 AN1; DATE: \$1/2001 11:0000 AN1; DATE: \$1/2001 AN1; D	Report Date: 3/7/2021 1:579/3PM Eastern Standard Time Report Date: 3/1/2021 1:57/47PM Eastern Standard Time	Event Details Event Details	2/11/2021 Access firented 2/11/2021 Access Granted	2513/3698 341906668	38 39
On Gound 7.4 Access Standard by Reader CURRY: START DATE: \$1/2001 1290000 AM; END DATE: \$3/2/2011 115555 PM; READERS: DAXY-1.2.03-PRCU P RAININ, DAXY-1.2.04-PREARMOON ENT	Report Date: 3/2/2021 1:57:42PM Seatern Standard Time	Event Details	2/11/2021 Access Granted	251308698	40
GROUND 3 A. Annua Company by Burder DI ERY: STAIT DATE: 2/1/2011 12:00:00 AM: END DATE: 3/2/2021 11:59:59 PM: READERS: DAXY-1.2.03-PICK UP RM IN, DAXY-1.2.0	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details Event Details	2/11/2021 Access Granted 2/11/2021 Access Smitted	289219548 341905470	41 42
OnStant 7.4 Recess Sented by Reader QUERY: START DATE: 2/2/20112:0000 AM; BND DATE: 3/2/201118955 PM; READERS: DATG-12.01-PKX UP RM IN, DATG-12.01-READSDOM ENT ORGANIC 7/4 Access Sented by Reader QUERY: START DATE: 2/2/20112:0000 AM; BND DATE: 3/2/201115:959 PM; READERS: DATG-12.03-PKX UP RM IN, DATG-12.04-REPARSDOM ENT	Report Date: 3/2/2021 1:57:42FM Sastem Standard Tinte	Event Details	2/11/2021 Access Granted 2/11/2021 Access Granted	410428965 295858646	43 44
On General 7.4. An excess Granted by Renders ONLERS' STATE (DATE: \$1/1/2021 12:00:00 AM); END DATE: \$1/1/2021 11:5955 PM; READERS: DAZO-12.03-PCK UP RIM IN, DAZY-12.04-READERSON NEW TOTAL TO A PROPERTY CONTROL OF THE PROPE	Report Date: 3/2/2021 1:579(2PM Easters Standard Time Report Date: 3/2/2021 1:579(2PM Eastern Standard Time	Event Datells	2/11/2021 Access Granted	34190666B	45
Officiard 7.4 Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/2/2021 13:59:59 PM; READERS: DATT-12:09-PICK UP RM IN, DATT-12:00-PICK UP RM IN, D	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time Report Date: 9/2/2021, 1:57:42PM Eastern Standard Time	Event Details	2/11/2021 Access Grented 2/11/2021 Access Grented	436135720 341925284	46 47
ORGAND 7.A ACCESS GENERALD PROJECT START DATE: 2/1/201120000 AAS, BID DATE: 1/2/2011150000 PAN, BEADERS; DATE:1.20-PICK UP RM IN, DOXT-1.20-69EAXSDOM ENT ORGAND 7.A Access Generald by Reader CUERY: START DATE: 2/1/201120001 AAS; BID DATE: 1/2/201115000 PAN, READERS; DATE:1.20-PICK UP RM IN, DOXT-1.20-69EAXSDOM ENT	Report Date: 3/2/2021 1:57/42PM Eastern Standard Time Seport Date: 3/2/2021 1:57/42PM Eastern Standard Time	Event Details	2/11/2021 Access Granted 2/11/2021 Access Granted	159466043 341524558	48 49
Ondarid 7.4 Access Granet by Reader QUERY: STAT DATE: 2/4/2021 12:00:00 AM; END DATE: 3/4/2021 11:09:59 PM; RENDERS: DAX7-12:03-PMC UP RAIN, DAX7-12:04-BREAKROOM ENT Ondarid 7.4 Access Granet by Reader QUERY: STAT DATE: 2/4/2021 12:00:00 AM; END DATE: 3/4/2021 11:15:029 PM; RENDERS: DAX7-12:03-PMC UP RAIN, DAX7-12:04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:43PM Eastern Standard Time	Event Details	2/11/2021 Access Granted	341813387	50
On Suand 2.4 Access Sciphad by Reeder QUERY: START DATE: 1/1/2011 12:000D NM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2:09-PICK UP RM W, DAX	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time Report Date: 5/2/2021, 1:57:42PM Eastern Standard Time	Eyent Detoils Eyent Details	2/11/2021 Access Granted 2/11/2021 Access Spanish	43.0429943 261668595	\$1 52
Undamed 7.4 Access General by Render CASERY: TRANT DATE: 1/1/2011 12:0000 AM; END DATE: 1/2/2011 11:00:00 PM; RELDERS: DATE-1.2:00-PKX UP RM IN; DRUT-1.2:00-REFARDOM ENT ORGANI 7.4 Access General by Render CASERY: STANT DATE: 1/1/2011 12:00:00 AM; END DATE: 1/1/2011 13:00:00 PM; READERS: DAVE-1.1:00-PKX UP RM IN; DRUT-1.2:00-REFARDOM ENT	Suport Date: 3/2/2021 1:57:42FM Fasture Standard Time	Event Details Event Details	2/11/2021 Access Granted 2/11/2021 Access Granted	172905979 341906668	33 54
Ordinard 7.4 Access Granted by Resider CULETY, START DATE: 2/1/2011 12:00:00 Aid; END DATE: 3/1/2011 11:56:29 PM; READERS: DAYS-1.2.0-PHCUE PRAIN, DAY	Report Date: 3/2/2021 1:57:42PM Seatern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datells	7/11/2021 Access Granted	289219546	56
THE PARTY OF THE P	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/11/2021 Access Granted 2/11/2021 Access Branted	290589060 290589060	56 57
Opiosate 7.4 Access Generaled by Revolution Opiosate 7.4 Access Ge	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Everyt Details	2/11/2021 Access Granted 2/11/2021 Access Granted	410432873	58 59
On General 7.4 Access Granted by Reader QUERY: STARY DATE: 1/1/2011 12:00:00 AM; END DATE: 3/2/2021 13:58:58 PM; READERS: QAX7-1.2.G-PICK UP RMIN, DAX7-1.2.G-PICK UP RMIN,	Report Date: 2/2/2021 1:57:42PM Fantern Standard Wine Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/11/2023 Access Granted	341922970	60
One-sand 7.4 Acoust Granted by Reader QUERY: START DATE: 2/1/2021 12/98/00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXY-1.2.09-PXX UP RM IN, DAXY-1.2.09-ESAUDOM-ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datells Event Datells	2/11/2021 Access Granted 2/11/2021 Access Granted	252615484 285215546	6) 62
Driguerd 7.4. Access Granted by Reader ULEN': START DATE: 27/3021 12:00:00 AM; END DATE: 37/2021 11:00:00 FM; McALENE; UAX-1_LUN-FAX OF ROWIN, DAX'-1_LUN-FAX DATE: 07/2021 12:00:00 AM; END DATE: 27/2021 11:00:00 FM; McALENE; UAX-1_LUN-FAX OF ROWIN, DAX'-1_LUN-FAX DATE: 07/2021 12:00:00 AM; END DATE: 27/2021 11:00:00 FM; McALENE; UAX-1_LUN-FAX DATE: 07/2021 12:00:00 AM; END DATE: 27/2021 11:00:00 FM; McALENE; UAX-1_LUN-FAX DATE: 07/2021 12:00:00 AM; END DATE: 27/2021 11:00:00 FM; McALENE; UAX-1_LUN-FAX DATE: 07/2021 12:00:00 AM; END DATE: 27/2021 11:00:00 FM; McALENE; UAX-1_LUN-FAX DATE: 07/2021 12:00:00 AM; END DATE: 27/2021 11:00:00 FM; McALENE; UAX-1_LUN-FAX DATE: 07/2021 12:00:00 AM; END DATE: 27/2021 11:00:00 FM; McALENE; UAX-1_LUN-FAX DATE: 07/2021 12:00:00 AM; END DATE: 07/2021 12:00 AM; END DATE	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/11/2021 Access Grunted 2/11/2021 Access Granted	341908422 134681775	6. 64
OnGuard 7.4 Access Grented by Reader QUERY: START DATE: 2/1/2011 12:00:00 AM; END DATE: 3/1/2011 11:59:59 PM; READERS: DAXT-1.2:08-PICK UP FIMIN, DAXT-1.2:0	Report Date: 3/2/2021 1:57:42PM Seatern Standard Time Report Date: 3/2/2021 1:57:42PM Seatern Standard Time	Eyent Details	2/11/2021 Access Granted	410429965	€5
20 Sept. 2015 PARTY STATE 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11:59:59 PM: READERS: DAX2-1.2:00-PICK UP RM IN, DAX7-1.2:04-BREAKROOM ENT	Report Date: 3/2/2021 1:57-42PM Seaturn Standard Time Report Date: 3/2/2021 1:57-42PM Eastern Standard Time	Event Details Event Details	2/11/2021 Access Granted 2/11/2021 Access Granted	251234739 261672745	66 67
On Source 2.4 Access (Secretar by Reader Objects: \$1/2001 12:00:00 AM; END DATE: \$1/2/2011 12:939 PM; READERS: DAVO-1.2.07-PICK UP RIMIN, DAVI-1.2.0-46/EARSDOOM ENT On Source 2.4 Access (Secretar by Reader Objects: \$1/2001 12:00:00 AM; END DATE: \$2/2/2011 13:939 PM; READERS: DAVO-1.2.07-PICK UP RIMIN, DAVI-1.2.0-48/EARSDOOM ENT ON THE \$1/2/2011 13:00 AM; END DATE: \$2/2/2011 13:939 PM; READERS: DAVO-1.2.07-PICK UP RIMIN, DAVI-1.2.0-48/EARSDOOM ENT ON THE \$1/2/2011 13:00 AM; END DATE: \$2/2/2011 13:00 AM; END DATE: \$1/2/2011 13:00 AM; END DATE	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 9/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/11/2021 Access Granted 2/11/2021 Access Granted	34 1908470 23 27 63 7 2 2	64
OnSuard 7.4 Access Graphed by Reador College; 27(2):221 (2):200.00 AM; BIO DATE: 27(2):221 (1):25:25 PM; BEADERS: DATE-1.22-PMCK UP SM IN, DAY-1.2-D-REAGROOM BYT COLLEGE; 27(2):221 (1):25:25 PM; BEADERS: DAY-1.2-2-PMCK UP SM IN, DAY-1.2-D-REAGROOM BYT COLLEGE; 27(2):221 (1):25:25 PM; BEADERS: DAY-1.2-2-PMCK UP SM IN, DAY-1.2-D-REAGROOM BYT COLLEGE; 27(2):221 (1):25:25 PM; BEADERS: DAY-1.2-2-PMCK UP SM IN, DAY-1.2-D-REAGROOM BYT COLLEGE; 27(2):221 (1):25:25 PM; BEADERS: DAY-1.2-2-PMCK UP SM IN, DAY-1.2-D-REAGROOM BYT COLLEGE; 27(2):221 (1):25:25 PM; BEADERS: DAY-1.2-D-REAGROOM BYT COLLEGE; 27(2):221 (1):25:25 PM; BEADERS: DAY-1.2-D-REAGROOM BYT COLLEGE; 27(2):25:25 PM; BEADERS: DAY-1.2-D-REAGROOM BYT COLLEGE; 27(2):25:25:25 PM; BEADERS: DAY-1.2-D-REAGROOM BYT COLLEGE; 27(2):25:25 PM	Report Date: 3/2/2021 157542PM Eastern Standard Time	Event Details	2/11/2021 Agents Granted	341911448	70
On Suand 7.4 Access Southed by Reader: QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 12:00:00 AM; READERS: DATE: 1/2/2021 12:00:00 AM; END DATE: 3/2/2021 12:00:00 AM; READERS: DATE: 1/2/2021 12:00:00 AM; END DATE: 3/2/2021 12:00 AM; END DATE: 3/2/2021 AM;	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Dutells Event Dutells	2/15/2021 Amess Granted 2/11/2021 Access Granted	218611830 341906658	71 72
75 STATE OF THE PROPERTY OF TH	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Dotelle Event Dotelle	2/11/2021 Access Grented 2/11/2021 Access Smated	241906734 232746517	79 74
Drisumer 7.4 Access Sentend by Resider CuERN: STAAT DATE: 2/1/2021 12:00:00 AM; END DATE: 3/7/2021 11:00:00 PM; READERS; DAXT-1.2.00-PICK UP RM M; D	Report Date: 3/2/2021 3:57:42PM Eastern Standard Time	Event Details	2/11/2021 Access Granted	251208941	75
OFFICE A SCALE CONTROL OF REALITY START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 12:55:59 PM; READERS: DAX7-1.2:03-PICK UP RM IN, DAX7-1.2:04-PREARROOM ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time Report Date: 9/2/2021, 1:57:42PM Eastern Standard Time	Event Details	2/11/2021 Access Granted 2/12/2021 Access Granted	245135872 341913887	76
On Guard 7.A. Access Granded by Resider QUERY: START DATE: 2/1/20112-90000 AM; 900 DATE: 2/1/201113-93-9 PM; READERS: DAVD-12-03-9/CX UP BM IN, DAV7-12-04-885ANNOOM ENT ORDERS 7.ACCESS GRAND BM CONTROL OF THE PROPERTY OF T	Report Date: 3/2/2021 1:57:40PM Eastern Standard Time	Event Details Event Details	2/52/2021 Access Granted 2/12/2021 Access Granted	436174709 341925272	2
OnGated 7.4 Access Greened by Reader CUERY: STAY DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:00:00 PM; RADDERS: DAX7-1.2.00-PICK UP RM IN, DAX7-1.2.00-BREADENCOM ENT CHEMICAL Access Greened by Reader CUERY: STAY DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:00:00 PM; READERS: DAX7-1.2.00-PICK UP RM IN, DAX7-1.2.00-BREADENCOM ENT	Report Date: 3/2/2021 2:57:42PM Fasturn Standard Time Report Date: 3/2/2021 2:57:42PM Eastern Standard Time	Event Details	2/12/2021 Access Granted	341920040	4
Dissuant 7.4 Access Symptot by Reader QUERY: START DATE: 2/1/2021 12:EDXD AN; SND DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2.03-PICX.UP RIAIN, DAX7-1.2.04-READERS CONTROL OF THE PART	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/12/2021 Access Granted 2/12/2021 Access Sminted	241910920 341520367	6
Command by Sender Country State DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1,2:03-9/0X 1P RM IN, DAX7-1,2:04-98EA0000M ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details		436)77783	8
Ordinard 7.4 Access Granted by Render OURSEY: START DATE: 2/1/2001 12:00:00 AM; END DATE: 3/2/2001 11:00:00 FM; REDDERS: DATE-1,2:00-PKC UP RM IN,	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datel	2/12/2021 Acous Granted	941892062 941892233	7D 8
Oxiguard 7,4 Access Grented by Reader QUERY: START DATE: 2/1/2011 21:90:00 AM; ENO DATE: 3/2/2011 11:59:59 PM; READERS: DATO: 4.201-12.01-10.01-12.01-10.01-12.01-10.01-12.01-10.01-	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details		341890454	11
CHIEFY: CTART DATE: 2/1/2021 17:00:00 AM: FMD DATE: 3/2/2021 11:59:59 FM: READERS: DATE: 3/2/2021 11:59 FM: READERS: DATE: 3/2/2021 11:59 FM: READERS: DATE: 3/2/2021 11:59 FM: READERS: DATE: 3/2/2	Report Date: 3/2/2021 1:57:43PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datali	2/12/2021 Access Granted 2/12/2021 Access Granted	341924338 341925548	13
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DISSURED A Access Granded by Resider CUSPY: START DATE: 7/1/2021 12;00:00 AAA; END DATE: 1/2/2021 11:95:59 PM; READERS: DACY-1.2.02 PKC UP RM IN; DATE-1.2.04-READERS: DACY-1.2.02 PKC UP RM IN; DATE-1.2.04-READERS: DACY-1.2.06-PKC UP RM IN	Report Date: 3/2/2021 1:57:42PM Sestem Standard Time	Event Detail	 2/12/2021 Access Strenbed 	341892084 341892767	21 32
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Undersid 7.4 Access Company by Render CURRY: 57AeT DATE: 2/1/2001 12:00:00 AM; SNO DATE: 3/2/2001 11:56:39 PM; SEADURS: DATE-12/1-10 PK; URRY: 1-1/2-00 PK; URRY: 1-1	Report Date: 3/2/2021 1:57:429M Eastern Standard Vinne Report Date: 3/2/2021 1:57:42PM Eastern Steadard Time	Event Data Event Data	la 2/13/2021 Access Granted	341925850	38
On Sugard 7.4 Access Granted by Render QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:39:59 PM; READERS: OAXY-11:20-PICKU PM NI, DAYY-12:204-REARROOM ENT	Report State: 2/2/2021 1:57:42PM Eastern Standard Time Report Date: 2/2/2021 1:57:42PM Eastern Standard Time	Event Deta Event Deta		341882414 341883520	39 40
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On Guard 7.4 Access Streeted by Reader QUERY: START DATE: 2/1/201112:00:00 AM; END DATE: 3/2/202111:59:59 PM; READERS; DAYY-1.2:09-PICK UP RM IN, DAYY-1.2:00-READERDOWN EN	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Seatom Standard Time	Event Deb Event Deb	As 2/12/2021. Access Grants	266335433	44 45
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Operand 7.4 Access Granted by Reader QUERT: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:00:00 PM; READERS: DAY:12:00-9/00.UF RM IN, DAY:7-1, 2:00-9/00.UF RM IN, DAY:7-1, 2:00-9/0	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Tico Report Date: 3/2/2021, 1:57:42PM Eastern Standard Tim	Event Det	ulls 2/12/2021 Access Grants	d 241892084	53
OnGused 7.4 Access Granded by Beatler QUERY: START DATE: 1/1/2021 12:50:001 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-12.00-PICK UP RM IN, DAX7-12.00 REPEARCOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Tim Report Date: 3/2/2021 1:57:42PM Eastern Standard Tim	Event Det	ulls 2/12/2021 Access Branks	d 341508568	54 55
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ORGANICITY A ROSES GRANARD BY REMONE COURTY: START DATE: 2/1/2021 120000 AM; END DATE: 3/1/2021 11:59:99 PM; ENDORS: DAX7-12:02-PM; DAX7-12:0	Report Date: 3/2/2021 2:57:42FM Eastern Standard Tim Report Date: 3/2/2021 2:57:42FM Eastern Standard Tim	Pvent Det			57 58

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Original 7.4 Access Granted by Reader QUERY: START PATE: 1/1/2021 12:00:00 AM; BNO DATE: 3/1/2021 11:59:59 PM; REFORMS: DATE-12.09-FICK UP RM IN; DATE-12.04-BREAKBOOM ENT OF BMIN 7.4 Access Granted by Reader: QUERY: START PATE: 2/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:59:59 PM; READERS: DATE-12.03-PMC UP RM IN; DATE-12.04-BREAKBOOM ENT	Report Date: 3/2/2021 157/42PM Eastern Standard Time E	rent Details	2/12/2021 Access Granted	251309818 260218731	80 61
OnSuard 7.4 Access terrated by Reeder QUERY, START DATE: 2/1/2021 12:00:00 AM; EMD DATE: 3/2/2021 13:00:00 PM; READING: DADY-1.2.03-PICU PM HIN, DAY-1.2.03-PICU PM HIN, DAY-1	Report Date: 3/2/2021 1:57:62PM Eastern Standard Time E	rent Details	2/12/2021 Access Granted 2/12/2021 Access Granted	732768722	62 63
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Origant 7.4 Access Granted by Beacher QUERY: START DATE: 2/1/2011 12:00:00 AM; END DATE: 3/1/2011 11:00:05 FM; READERS: DADY-1.201-FRCU PRAIN, DAY-1.204-READERS DATE: 10:00 AM;	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time E	yent Datalis	2/12/2021. Access Granted 2/12/2021. Access Granted	42 615 CFC3 34 19 CF568	67 68
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Option 7.4 Access General by Reader Outlier: 57470021 (2000) and the 3/2/2021 (15959) PM: REAGES: DAY-12.03-PXC UF BM IN, DAX7-12.03-PXC UF BM IN,	Report Date: 3/2/2021 LS73/2PM Eastern Standard Time	vent Details	2/12/2021 Access Grented 2/13/2021 Access Grented	341906734 410491727	74
Ordigard 7.A. Access Granted by Render: QUERY, START DATE: 2/1/2021 12:0000 ART: 8/2/2021 11:55:59 PM; READER: DATY-1.2:00-PMC UP BM R, DOCY-1.2:00-BEARROOM ENT	Report Date: 3/2/2021 1:57:42PM Fastern Standard Time	vent Details	2/13/2021 Actes Granted	343820062 343820040	2
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CONTRACTOR OF STATE DATE: 20/2021 12-0020 AM: FND DATE: 3/2/2021 11:58:58 PM; READERS; DAX7-1-2:08-PKX UF RM IN, DAX7-1-2:	Report Date: 3/2/2021 1:57562FM Eastern Standard Time	ivant Detells Ivent Detells	2/13/2021 Access Granted 2/13/2021 Access Granted	436174753 436177799	6
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OnStated 7.4 Access Granted by Resides QUERY: STATE DATE: \$\(2\subseteq \) 120021 12:00001 ANI; EMD DATE: \$\(3/\subseteq \) 120021 13:0000 PM. \$\(4\subseteq \) 120021 13:00000 PM. \$\(4\subset	Report Date: 3/1/2021 1:57:42PM Eastern Standard Time	Event Details	2/13/2021 Access Granted 2/13/2021 Access Granted	34 1919556 436174709	11 12
Optional 7.4 Access Granted by Reader CLIERY: START DATE: 2/1/20/1 12:00:00 AM; END DATE: 2/2/2021 11:59:59 FM; READERS: DADY-1.2.03-FICK UP RIMIN, DATE: 1/2/20/1 12:00:00 AM; END DATE: 3/2/2021 11:59:59 FM; READERS: DADY-1.2.03-FICK UP RIMIN, DATE: 1/2/20/2 III. START DATE: 2/1/20/2 III. START DATE: 2/1/20/2 III. START DATE: 3/2/2021 III. START DATE: 3/	Report Date: 8/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/13/2021 Access Grented	341913079	13
On Guesti 7.4 Access Granted by Reader QUERY: START DATE: 7/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXY-1.2.GE-PKX IP NM IN, UNC.1-1.2.GE-PKX IP NM IN UNC.1-1.GE-PKX IP NM IN UNC.1-1.2.GE-PKX IP NM IN UNC.1-1.2.GE-		Event Details Event Details	2/13/2021 Access Granted 2/13/2021 Access Granted	341912991 341922904	14 15
On General JA. Access Generated by Namedra CULENT: STANT DATE: 21/2021 12:00:00 AM; DID DATE: 37/2021 11:50:50 PM; REVERSE: DATE: 12-00-PMX UP RAINI, DATE: 12-00-PMX UP RAINI	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Getalis Event Datalis	2/13/2021. Access Grented 2/13/2021. Access Grented	436178476 341689306	16 17
ORGANIC T.A. Access Granted by Resider QUERY: START DATE: 2/1/2021 12:00:00 AM: BY LIARTE: 3/2/2021 13:00:00 PM; READERS: GANT-LIARTE GANT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/19/2021 Access Granted 2/19/2021 Access Granted	341920107 436178482	18 19
OnGuard 7.4 Access Generally Reader QUERY: STATI DATE: \$\frac{1}{2}\sqrt{1}\text{2}\text{2}\text{1}\text{2}\text{2}\text{2}\text{2}\text{2}\text{2}\text{3}\text{1}\text{3}\text{1}\text{2}\text{3}\text{2}\text{2}\text{3}\text{1}\text{3}\text{1}\text{3}\text{5}\text{2}\text{3}\te	Report Date: 3/2/2021, 1:57:s2PM Eastern Standard Time	Event Details	2/13/2021 Access Granted 2/13/2021 Access Granted	341825627 341891093	20 21
OnGustri 7.4 Access Granted by Randar QUSERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:50:55 PM; READERS: DAGF-12:00-912 UP RM IN, DAX7-12:00-912 UP RM IN,	Report Date: 5/2/2021 1:57:42PM Eastern Stundard Tiret	Event Datalls Event Datalls	2/13/2021 Access Grented	436174401	22
CINGUIRD 7.4 Access Greeked by Render CULERY: START DATE: 2/1/2021 12:00:00 AN; END DATE: 3/2/2021 11:59:59 PM; READERS: DAZY-1.2:03-FIXE IP RM IN, DAZY-1.2:03-FIXE IP RM IN, DAZY-1.2:03-FIXE IP RM IN, DAZY-1.2:03-FIXE IP RM IN CONTAINED I	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/13/2021 Access Granted 2/13/2021 Access Granted	436174665 341892282	23 24
On-Guard 7.4 Access Generate Printed Culter: STANT DATE: 2/1/2021 12:00:00 AM; DND DATE: 3/1/2021 115:529 PM; REPUBRIS: DATE: 1.2.01-PMCK UP RM IN; DND DATE: 1.2.01-PMCK UP RM IN; DND DATE: 3/1/2021 115:529 PM; REPUBRIS: DATE: 1.2.01-PMCK UP RM IN; DND TATE: 1.2/1/2021 115:529 PM; REPUBRIS: DATE: 1.2.01-PMCK UP RM IN; DATE: 1.2.01-PMCK	Report Date: 3/2/2021 1:57:42PM Seaters Standard Tires Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/13/2021 Access Granted 2/13/2021 Access Granted	436),74721 241692569	25 26
Chisard 7.4 Access Gretted by Render Cultury: START DATE: 2/1/2011 12:0000 AM; BND DATE: 3/1/2011 11:9559 PN; READERS: DAVI-1.20-9 PIX UP SM HI, DAV	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datella Event Details	2/19/2021 Access Granted 2/13/2021 Access Granted	341906144 341519622	27 28
Gridward 7.A Access Strained by Reader CHERT: START DATE: 2/1/2001 12:00000 AN; EMD DATE: 3/2/2001 11:58:59 PM; EAGDERS: DAX7-12.00-PMCK UP RM IN;	Report Date: 3/2/2021, 1:57:02PM Fastern Standard Time Report Date: 3/2/2021, 1:57:02PM Factorn Standard Time	Event Details	Z/13/2021 Acous Granted	341891539 341919544	29 30
On Guard 7.4 Access Granted by Render QUERY: START DATE: 2/1/2021 12:00:00 AM; SHO CATE: 3/2/2021 11:59:55 PM; READERS: DAGG-1-2.03-PICK UP RIM IN, DATE-1-2.04-PREADERS UNIVERSITY OF THE PROPERTY OF THE PRO	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/13/2021 Access Granted 2/13/2021 Access Granted	43.5193060	21
OnGuard 7.4 Access Granded by Reader: QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PA; READERS: DAXT-12.03-PROX UP RM IN, DAXT-1.2.03-PROX UP RM IN, DAXT-1.2.0	Report Date: 3/2/2021 1:57502PM Eastern Standard Time Report Date: 5/2/2021 1:57502PM Featern Standard Time	Event Details	2/19/2021 Access Granted 2/12/2021 Access Granted	341913431 341892084	32 33
ORGANIC 7.4 Access Generated by Reador CLOSEY: START DATE: 2/1/2021 12:00:00 ANC; DED DATE: 3/1/2021 13:00:00 PM; READERS: DATE-1,20-PMC UP RM IN, DAT	Report Date: 3/2/2021 1:57/42PM Eastern Standard Time Report Date: 3/2/2021 1:57/42PM Eastern Standard Time	Event Dotalis Event Details	2/13/2021 Access Scented 2/13/2021 Access Granted	341925218 341812682	34 35
Originat 7.4 Access Sented by Reader QUERY: STAIT DATE: 2/1/2011 12:00400 Abt; END DATE: 3/1/2011 11:59:59 PM; READING: DATE: 12:00-9012 UP NR IN, DATE: 12:00-9012 Abt; END DATE: 3/1/2011 11:59:59 PM; READING: DATE: 12:00-9102 PM; DATE: 12:00-9012 PM; DATE: 12:	Report Date: 3/3/2021 1:57:42PM Eastern Standard Time	Event Details	2/13/2021 Access Granted 2/13/2021 Access Granted	410482873	36 27
DriGuerd 7.4 Access Granted by Reader QUERY: START DATE: \$2/3/2011 12:0000 ANY, 19th DATE: \$2/3/2011 11:5959 PM; READERS: DAXY-1_2.00-FICK UP RN IN, DAXY-1_2.00-REARROOM ENT	Report Date: 3/1/2021. 1:57:42PM Eastern Standard Time Report Date: 3/1/2021. 1:57:42PM Fastern Standard Time	Event Cetalls	2/12/2021 Access Stanted 2/12/2021 Access Stanted	341852326 341922970	38 39
OnGlastd 7.4 Access Stanted by Reader QUERY; START DATE: 2/1/2021 12:00:00 AM; BND DATE: 3/1/2021 11:59:59 PM; READERS: DAX/-17:09-PICK UP (MIN), DAX/-1-2:09-PICK UP (MIN), DAX/-1-2:0	Report Date: 9/2/2021 1:57:42PM Seaters Standard Time Report Date: 3/2/2021 1:57:42PM Seaters Standard Time	Event Details	2/13/2021 Access Granted	341925950	40
Originard 7.4 Accepts Granted by Reader OWERY; STARU DATE: 2/1/2021 12:0000 AM; EMD DATE: 3/2/2021 11:00:00 AM; EMD DATE: 3/2/2021 11:00 AM; EMD DATE: 3/2/2021 11:00 AM; EMD DATE: 3/2/2021 III AM; EMD DATE: 3/2	Report Date: 3/2/2021 1:57-4/2PM Eastern Standard Three Saport Date: 3/2/2021 1:57-4/2PM Eastern Standard Time	Event Details	2/13/2021 Access Granted 2/13/2021 Access Granted	341881731 436152950	41. 41
On Search 7.4 Access Gambad by Render QUERY: START DATE: 2/17/02/12/2000 ANY, SHULLINE: 2/17/02/11/15/55 PM: READERS: DAXY-12/05-PICK UP RININ, DAXY-12/05-BEEAKNOOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datalis Event Datalis	2/19/2021 Access Granted 2/19/2021 Access Granted	241584594 341908866	43 44
Owdurd 7.A. Access Granted by Reader Confess C	Report Date: 3/1/2021 1:57:42PM Eastern Standard Tires Report Date: 3/1/2021 1:57:42PM Eastern Standard Tires	Event Details Event Details	2/13/2021 Access Granted 2/13/2021 Access Granted	341925284 341891665	45 46
On Baserid 7.4 Accept Granted by Reader QUERY: START DATE: 3/1/2011 12:00:00 AM: BND DATE: 3/1/2011 13:59:59 PN; READERS: DATE-1.205-FICK IP RMM, QAST-1.204-REAGROUDIM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/13/2021 Access Granted 2/13/2021 Access Granted	261665595 341908685	47 48
Directory A Access Greated by Render CURRY, START DATE: 2/J/2021 125000 AM; ND DIRECT 2/J/2021 125500 PM; READERS: DAXY-1.2,05-HEX.UP RM, DAXY-1.2,05-HEXADOLINA END DIRECT 2/J/2021 125500 PM; READERS: XXXY-1.2,05-HEXADOLINA END DIRECT 2/J/2021 125500 PM; READERS: XXXY-1.2,05-HEXADO	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/13/2021 Access Granted	341891781 341891952	49 50
OnSound 7.4 Account Secretary is Start DATE: 2/1/2021 12:00:00 AM; SND DATE: 3/1/2021 11:55:59 PM; REDDERS: 00:00-1-2021-10:00 MR IN, DATE-12:00-98-58-88-00 MR IN	Report Date: 3/2/2021 1:57x12PM Eastern Standard Time Report Date: 3/2/2021 1:57x12PM Eastern Standard Time	Event Details Event Details	2/12/2021 Access Granted 2/13/2021 Access Granted	436178432	51
Origuard 7.4 Access Granted by Resider OCUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:58 FM; READERS: DAXY-1.2:09-PICK UP MAIN,	Report Date: 3/2/2023, 1:57542PM Eastern Standard Titror Report Date: 3/2/2023, 1:57:42PM Eastern Standard Titro	Event Details Svent Details	2/15/2021 Azona Granted 2/13/2021 Acons Granted	410432873 175916200	52 53
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On Spannid 7.4 Access Stephad by Header QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:50 PM; READERS: 00:07-12:08-9M R), DRIVE 19:00 PM R), DRIVE	Report Date: 2/2/2021, 1:57:42PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details	2/13/2021 Access Granted	2927@722 341906470	58 59
Origizard 7.4 Access Granted by Readur QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:59:59 PM; READERS: DAXF-1.2:03-PICK UP RIGH N, GAXT-1.2:04-BEARDOOM ENT	Report Date: 9/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details		341919864	60
THE REAL PROPERTY OF THE PARTY	Report Date: 3/2/2021 1:57:42PM Eastern Stendard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Getails		341925923	61 62
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OmGuard 7.4 Access Granted by Reader LUKEN: 57ART DATE: 1/1/2011 12:00:000 AM; BH/D DATE: 2/1/2021 11:59:00 PM; EMPARE: DAVE-1_2.00-01 CUFF MIN, LOW-7_1_0.00-000 AM; BH/D DATE: 2/1/2021 11:59:00 PM; EMPARE: DAVE-1_2.00-000 AM; LOW-7_1_0.00-000 AM; BH/D DATE: 2/1/2021 11:59:00 PM; EMPARE: DAVE-1_0.00-000 AM; LOW-7_1_0.00-000 AM; BH/D DATE: 2/1/2021 11:59:00 PM; EMPARE: DAVE-1_0.00-000 AM; LOW-7_1_0.00-000 AM; BH/D DATE: 2/1/2021 11:59:00 PM; EMPARE: DAVE-1_0.00-000 AM; LOW-7_1_0.00-000 AM; BH/D DATE: 2/1/2021 11:59:00 PM; EMPARE: DAVE-1_0.00-000 AM; LOW-7_1_0.00-000 AM; BH/D DATE: 2/1/2021 11:59:00 PM; EMPARE: DAVE-1_0.00-000 AM; LOW-7_1_0.00-000 AM; BH/D DATE: 2/1/2021 11:59:00 PM; EMPARE: DAVE-1_0.00-000 AM; BH/D DATE: 2/1/2021 11:59:00 PM; EMPARE: DATE:	Report Date: 3/2/2021 157:42PM Eastern Stendard Time	Event Detail	2/13/2021 Access Granted	251308941 289216484	67 68
OnSOLVE 7.4 Access Started by Reader CURENY: START DATE: 1/1/2013 12:00000 AN); END DATE: 1/1/2013 13:09:65 PM; REXPORTS: DAVE-12:00-PM; END DATE: 1/1/2013 13:09:000 PM; END DATE: 1/1/2013 13:000 PM; END DATE: 1/1/2013 13:09:000 PM; END DATE: 1/1/2013 13:000 PM; END DATE: 1/1/2013 13:09:000 PM; END DATE: 1/1/2013 13:09:000 PM; END DATE: 1/1/2013 13:09:000 PM; END DATE: 1/1/2013 13:000 PM; END DATE: 1/1/2013 13:09:000 PM; END DATE: 1/1/2013 13:000 PM; END	Report Date: 2/2/2021 1:57:42PM Seaturn Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datell	2/13/2021 Access Granted	282771323 28921A743	69 70
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THE PARTY NATIONAL PROPERTY AND CONTRACT FOR CONTRACT REPORTS DAYS -1.2.03-PICK UP RM IN, DAYS -1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detail		341926358 341925284	2
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Origuent 7.4 Access Standard by Reader OURIV: STANT DATE: 2/1/2001 12/2000 AME; END DATE: 3/2/2011 13/5/98 PM; RESURES: DATE-1_2-09-PIX UP RAMIN, DADY-1_2-04-886ARROOM ENT	Report Date: 3/2/2021 3:57:429M Eastern Standard Time Report Date: 3/2/2021 1:57:429M Eastern Standard Time	Event Detail	a 2/14/2021 Access Granted	941920107 341892062	6
Uniformed 7.6 Access Granted by Breider QUERY: START DATE: 2/1/2021 12:00:20 AM; END DATE: 3/1/2021 11:59:59 PM; READERS: DADO-12:23-FICEU PM IN, DAXY-12:24-ERANDOM ENT	Represe Date: 1/2/2021 1:57:42PM Eastern Standard Titra	Event Datal	s 2/14/2021 Access Grented	341919556 341925372	7
Origuird 7.4 Access Granted by Reader OURSY: START DATE: 2/1/201 12:00:00 ANI; SHD DATE: 3/2/201 11:59:58 PM; READER: DAY-12:00-PKC UP WARR, DOWN-12:00-PROMOMENT OF THE PROMOMENT OF THE PROMO	Report Date: 3/2/2021 1:57:42PM Septem Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detail	b 2/14/2021 Access Granted	436174709	10
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DpSumrd 7.4 Access Granted by Baseler QUERY: START DATE: 1/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:39:59 FM; READSS: DAXY-1.2.09-FM2 UP MAIRE, DAX/-1.2.09-FM2 UP MAIRE, DAX	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detail			12 13
Distance 7.4. Access Granted by Reader ORGANIC TA. Access Granted by	Report Date: 3/2/2021 157:42PM Sestem Standard Titre Report Date: 3/2/2021 157:42PM Sestem Standard Time	Event Detail			14 15
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Originard 7.4 Access Searced by Reader QUERY: START DATE: 21/2021 12:5000 AM: IRD DATE: \$2/2021 11:58:59 PM; READERS: DATE: 22.209-PDX: UP RW IV, DATE: 21/2021 12:5000 AM: DATE: \$2/2021 11:58:59 PM; READERS: DATE: 22.209-PDX: UP RW IV, DATE: 21/2021 12:5000 AM: DATE: 21/2021 11:58:59 PM; READERS: DATE: DATE: 20/2021 12:5000 AM: DATE: 21/2021 11:58:59 PM; READERS: DATE: DATE: DATE: 21/2021 11:58:59 PM; READERS: DATE:	Report Date: 3/2/2021 157/42PM Eastern Standard Time Report Date: 3/2/2021 157/42PM Eastern Standard Time	Event Data	is 2/14/2021 Access Grantee	436174731	1.0 1.0
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OnGuard 7.4 Access Granted by Read	QUERY: START OUTE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-L2:09-PICK DR RM IN, DAX7-L2	OI-BREAKROOM ENT Report Date: 9/2/2021, 157/42PM Eastern Standard Time	Event Details	2/15/2021 Access Granted	141890233	23
On Guard 7.4 Access Granted by Read On Guard 7.4 Access Granted by Read			Event Details Event Details	2/15/2021 Access Granted 2/15/2021 Access Granted	436192047 410493578	14 15
DoGward 7.4 Access Granted by Read	 QUERY; START DATE: 2/1/2021 12:00:00 AM; BN0 DATE: 2/2/2021 11:59:59 PM; READERS: DAID-1.2.03-PICK UP RM IN, DAID-1.2. 	DI-SREAKRODM ENT Report Date: 1/2/2021 1:37:42PM Eastern Standard Time	Event Details	2/15/2021 Acress Granted	341924336 436174476	15
On Guard 7.4 Access Granted by Read On Guard 7.4 Access Granted by Read			Event Detells Event Octobs	2/15/2021 Access Grented 2/15/2021 Access Grented	24 1920239	SIII
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OnGuard 7.4 Access Granted by Read OnGuard 7.4 Access Granted by Read			Event Datalis	2/25/2021 Access Granted	941891093	21
On Buard 7.4 Access Grented by Reed	 QUERY: START DATE: 2/1/2021 12/00:00 AM; END DATE: 1/2/2021 11:58:59 PM; READERS: DAX7-1.2:09-PICK UP RM IN, DAX7-1. 	.04-BREAKROOM ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/15/2021 Access Greeted 2/15/2021 Access Greeted	496174401 341890520	22 23
On Guard 7.4 Access Granted by Read On Guard 7.4 Access Granted by Read	ULBRY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 9/2/2021 11:59:59 PM; READERS: DAXY-1.2.03-PICK UP RIM IN, GAXY-1.2.	.04-87KAKRODM ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detells Event Detells	2/15/2021 Access Granted	341924402	24
On Guard 7.4 Access Granted by Read	FIGURERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/3021 11:59:89 PM; READERS: DAX7-12,03-PICK UP RM IN, DAX7-12	OF BREAKROOM ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detella Event Detella	2/15/2021 Access Granted 2/15/2021, Access Granted	341919644 436174731	25 26
OnGuerd 7.4 Access Granted by Read OnGuerd 7.4 Access Granted by Read			Brent Details	2/15/2021 Access Granted	341924094	27
On Guard 7.4 Access Granted by Read	If QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 9/2/2021 11:59:59 PM; READERS: DAX7-1.2:03-PKX 12P RM IN, DAX7-1.		Event Details Event Details	2/15/2021, Access Granted 2/15/2021, Access Granted	941926627 410433666	25 29
On Guard 7.4 Access Granted by Read On Blund 7.4 Access Granted by Read	START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:58:58 PM; READERS; DAX7-L2:08-PMCX UP RM W, DAX7-L	.04-BREAKROOM ENT Report Gate: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detalls	2/15/2021 Access Granted	341925218	30
OnGuard 7.4 Access Strented by Reed	W QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/3/2021 11:59:59 PM; READERS: DAX7-1.1.08-PICK UP RM IN, DAX7-1.		Event Details Event Details	2/15/2021 Access Granted 2/15/2021 Access Granted	341919622 341890988	31 32
OnGuard 7.4 Acress Granted by Resc OnGuard 7.4 Acress Granted by Resc	R QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAUG-1.2:03-PICK UP RM IN, DANT-1.	DE-BREAKROOM ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	7/15/2021 Accept Graphed	436174665	33
On Guard 7.4 Access Granted by Read Do Buard 7.4 Access Granted by Read		LDE-BREAKROOM ENT Report Date: 3/2/2021 157:42PM Sestem Standard Time LDE-BREAKROOM ENT Report Date: 3/2/2021 157:42PM Sestem Standard Time	Event Detells Event Detells	2/15/2021, Access Branted 2/15/2021, Access Branted	241892084 289222124	24 35
Driguard 7.4 Access Granted by Near Driguard 7.4 Access Granted by Near	 QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 8/2/2021 11:58:58 PM; READERS: DAX7-1-2-02-PICK UP RM IN, DAX7-1. 	LOU-BREAKROOM ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Evert Details	3/15/2021 Access Granted	341925350	36
On@und 7.4 Access Strented by Read On@und 7.4 Access Granted by Read	 QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/2/2021 12:9:59 PM; READERS: 0AX7-1.103-PKK UP RM IN; DAX7-1. QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2021 11:9:59 PM; READERS: GAX7-1.1.03-PKK UP RM IN; DAX7-1. 	LOS-BREAKROOM ENT Repurt Date: 3/2/2021 1:57-439M Eastern Standard Time LOS-BREAKROOM ENT Report Date: 9/2/2021 1:57-439M Eastern Standard Time	Event Details Event Details	2/15/2021 Access Granted 2/15/2021 Access Granted	941286757 341284594	27 38
On Guard 7.4 Access Granted by Reas	W QUERY: START DATE: 2/1/2001 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2:08-PICK UP RM IN, DAX7-1.	LOS-EREAKROOM ENT Report Date: 2/2/2021 1:57:42PM Eastern Standard Time	Event Detells	2/15/2021 Access Granted	341865807	39
On Buard 7.4 Access Granted by Read Dr. Buard 7.4 Access Granted by Read			Event Octobs Event Details	2/15/2021 Access Granted 2/15/2021 Access Granted	486174797 341925284	40 41
OnGuard 7.4 Access Scanted by Resi	er QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 9/2/1021 11:59:59 PM; READERS: DAXX-12.03-PICK UP RM IN, DAXX-1.	2.08-BREAKROOM ENT Report Date: 9/2/2021, 1:57:429M Eastern Standard Time	Event Details	2/15/2021 Access Stanted	289219546	42 43
OnGuard 7.4 Access Streeted by Rese OnGuard 7.4 Access Granted by Rese		2,04-BREAKROOM ENT Report Detet: 9/2/2021 1:57:429M Enstern Standard Time 1.04-BREAKROOM ENT Report Outer: 1/2/2021 1:57:429M Enstern Standard Time	Event Details Event Details	2/15/2021 Access Streeted 2/15/2021 Access Simpled	43 61929SG 26166959S	44
On Swand 7.4 Access Granted by Res	er QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 9/2/2021 11:59:58 PM; READERS; CAX7-1,2:09-PKX UP RM IN; DAX7-1.	LOS-BREAKROOM ENT Report Oate: 3/2/2021 1:57:42PM Eastern Standard Tires	Event Details Event Details	2/15/2021 Access Greened 2/15/2021 Access Greened	341906734 341906754	45 46
Driguerd 7.4 Access Grented by Resi Driguerd 7.4 Access Smitted by Resi		2.04-BRSACROOM ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/15/2021 Access Granted	172905979	47
GnGuard 7.4 Acress Stanted by Rea	Mr QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2:03-PICK UP RM IN, DAX7-1.	Z.DE-BREAKROOM ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/15/2021 Access Granted 2/15/2021 Access Branted	420491727	48 45
On Guard 7.4 Access Branted by Rea On Guard 7.4 Access Granted by Rea			Event Detells	2/15/2021 Access Branted	43.5192950	50
On Buard 7.4 Access Granted by Rea	(a) QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; RÉADERS: CIAXT-1.2:03-PICK MP RM IN, DAXT-1.		Event Datells Event Datells	2/15/2021 Access Granted 2/15/2021 Access Granted	43.6192950 134891775	51 52
On-Guard 7.4 Access Granded by Res On-Guard 7.4 Access Standed by Res		2_06-BREAKROOM ENT Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details	2/15/2021 Access Granted	251334739	58
On Guard 7.4 Acress Grented by Res	ler QUERY: STAUT DATE: 3/1/2021 12:00(X) AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXT-1.2:09-PICK UP NM IN, DAXT-1	2.08-BREAKROOM ENT Report Date: 3/2/2021 1:57:42PM Sestem Standard Time 2.08-BREAKROOM ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/15/2021 Access Granted 2/15/2021 Access Granted	34189639 341806822	54 55
On Guard 7.4 Access Granted by Rea On Guard 7.4 Access Granted by Rea		2.04-BREAKROOM ENT Report Date: 9/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/15/2021 Access Grented	438190703	56
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OnGuard 7.4 Access Granted by Ret OnGuard 7.4 Access Granted by Ret	Mr. QUERY: START DATE: 2/1/2021 12:00:00 AM; BND DATE: 3/2/2021 11:58:58 PM; READERS: DAX7-1.2.03-PICK UP RM IN, DAR7-1	2.04-BREAKROOM ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/15/2021 Access Granted	261872745	89
On Guard 7.4 Access Branted by Rea On Guard 7.4 Access Branted by Rea			Event Details Event Octalis	2/15/2021 Access Greated 2/15/2021 Access Greated	218611830 341881027	60 61
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On Busini 7.4 Access Granted by Rea	Hy CLUERY: START DATE: 2/1/2021 12:00:00 AM; BND DATE: 9/2/2021 13:58:58 PM; READERS: DAX7-1.2:03-PKK UP RM IN, DAX7-1	2.04-BREAKROOM ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detella	2/16/1021 Access Granted	341920040 341906470	2
Optioned 7.4 Access Granted by Res		2.06-BREAKROOM ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard Time 2.06-BREAKROOM ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/16/2021. Access Granted 2/16/2021. Access Stanted	3419216/F	Ä
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OrrGuard 7.4 Access Granted by Res On Quard 7.4 Access Granted by Res		.2.04-BREAKROOM ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard Tirse	Event Details	2/15/2021 Access Granted	341890434	7
On Buard 7.4 Access Granted by Re-	dw chiery: Start Date: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: IIAX7-1;2.03-PKK 1/P RM IN; DAUX-1		Event Datalis Event Datalis	2/15/2021 Access Granted 2/15/2021 Access Granted	436174709 341892063	8
On Guard 7.4 Access Granted by Re- Driguard 7.4 Access Granted by Re-	der Query: START DATE: 2/1/2021 12:00:00 AM; END DAYE: 3/2/2021 11:59:59 PM; READERS: DAXT-1.2.03-PICK UP RM IN, DAXT-	2.04-BREAKRUUM ENT Report Date: 3/2/2021 1:57:43* M Eastern Standard Time	Event Details	2/16/2021. Access Granted	341919586	10
On Guard 7.4 Access Stanted by Re On Guard 7.4 Access Granted by Re			Event Details Event Details	2/16/2021 Access Granted 2/16/2021 Access Granted	43-6192047 341924336	11 12
On Buard 7.4 Access Granted by Re	der OUERT: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:58 PM; READERS: DAXO-1.2:08-PICK UP RM IN, DAXO-	2.04-BREAKROOM ENT Report Oute: 2/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/15/2021 Access Granted 2/15/2021 Access Granted	241920107 436176476	13 14
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OnGuard 7.4 Access Granted by Re OnGuard 7.4 Access Granted by Re	IN CHERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXO-1.2:08-PICK UP RM IN, DAX7-	L2.04-BREAKBOOM ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/16/2021 Access Branked	341924402	18
On@uard 7.4 Access Granted by Re	INCHES CHIERY: START DATE: 1/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:58 PM; READERS: DAXD-1/2/08-PKX NP RM IN, DAXD-		Event Details Event Details	2/15/2021 Access Granted 2/15/2021 Access Granted	341919644 436174401	19 20
On Sunrd 7.4 Access Granted by Re On Suard 7.4 Access Granted by Re	OLIERY: START DAYE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXT-1.2.03-PICX UP RM IN, DAXT-	. 2.04-BREAKROOM ENT Report Date: 3/2/2021 1:57:43PM Eastern Standard Time	Event Details	2/16/2021 Access Granted	341926627	21
On Guard 7.4 Acress Granted by Re On Guard 7.4 Acress Granted by Re			Event Datella Event Datella	2/16/2021 Access Granted 2/16/2021 Access Granted	341890233 410433666	22 23
On Guard 7.4 Access Granted by Re	der Query: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2:09-PICK UP RM IN, DAX7-	1.2,04-BREAKROOM ENT Report Date: 1/7/2021 1:57:42PM Eartern Standard Time	Event Details	2/16/2021 Access Clarited	941892202	24
On Stand 7.4 Access Smitted by Re On Stand 7.4 Access Smitted by Re				2/15/2021 Access Granted 2/15/2021 Access Granted	341.892767 341.891533	25 26
DriGuard 7.4 Access Granted by Ru	Ider CUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAG-1-2:03-PICK UP RM IN, DAKT-	1.2.04-BREAKROOM ENT Apport Date: 3/2/2021 1:57:41PM Eastern Standard Tim	Event Details	2/16/2021 Access Granted 2/16/2021 Access Granted	341919622 341892084	27 28
OpGuard 7.4 Access Granted by Ru OmGuard 7.4 Access Granted by Ru	QUERY: START DATE: 2/1/2021 12:00:00 AM; BND DATE: 3/2/2021 11:50:50 PM; READERS: DAXT-1.2:03-PICK UP RM IN, DAXT-	1.2.04-BREAKROOM ENT Report Deto: 3/2/2021 1:57:42PM Eastern Standard Tim	Event Datallo	2/16/2021 Access Granted	458178432	29
OnGuerd 7.4 Access Granted by Re	oder Query: Start gate: 2/1/2021 12:00:00 am; end date: 2/2/2021 11:59:59 pm; readers: datg-1.2:09-pick up rm in, daxy-	1.2.04-BREAKROOM ENT Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details	2/15/2021 Access Granted 2/19/2021 Access Granted	426177838 436174665	30 21
On Guard 7.4 Access Granted by Ri UniQuard 7.4 Access Granted by Ri	eder CUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:50:50 PM; READERS: UAXD-1:2,03-PICX UP RM IN, DACC-	1.2.04-BREAKRODM ENT Report Date: 3/2/2021 1:57942PM Eastern Standard Tim	Event Details	2/16/2021. Access Granted	343090386	32
OnGuard 7,4 Access Granted by R	oder Cruery: START DATE: 2/1/2021 12:00:00 AM; EMD DATE: 1/2/1021 11:59:59 PM; READERS: DAXF-1.2:03-PICK UP RM:IN, DJ007-	1.2.04-BREAKROOM EN7 Report Date: 3/2/2021 1:579/2PM Eastern Standard Tim 1.2.04-BREAKROOM EN7 Report Date: 3/2/2021 1:57-42PM Eastern Standard Tim	Event Details Event Datails	2/16/2021 Access Granted 2/16/2021 Access Granted	341888757	33 34
On Guard 7.4 Access Granted by R On Guard 7.4 Access Granted by R	HONEY: STAIRT DATE: 2/1/2011 12:00:00 AM; END DATE: 3/1/2021 11:59:59 PM; READERS: DAX7-1,2:09-PICK UP RIM IN, DAX7-	1.2.04-BILEAKROOM BYT Report Date: 3/2/2021 1:57:42PM Eastern Standard Tim	Event Details	2/16/2021 Access Granted	341890520	35
OnGuard 7.4 Access Granted by R OnGuard 7.4 Access Granted by R	OLIERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2021 11:59:59 PM; READERS: DAUI-12:09-PICK UP RAI IN, DARF.	1.2.04-BREAKROOM ENT Report Date: 3/3/2021 1:57:43PM Eastern Standard Tire 1.2.04-BREAKROOM ENT Report Date: 2/1/2021 1:57:43PM Eastern Standard Tire		2/18/2021 Access Granted 2/18/2021 Access Granted	341,891731 289219546	26 27
On Sueed 7.4 Accum Grented by R	### GUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 9/2/2021 11:59:59 PM; READERS: DAW-1.2.03-PKK UP AM IN, GART	1.2.04-SREAKROOM ENT Report Date: 1/2/2021 1:57:42PM Castern Standard Titr	Event Dutelle	2/16/2021. Access Granted	436192950	38
On Guard 7.4 Access Granted by R On Guard 7.4 Access Granted by R				2/16/2021 Access Granted 2/16/2021 Access Granted	410433578 235638646	35 40
OnGuard 7.4 Access Granted by R	HIGHE QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:50:50 PM; READERS; DAX7-1.2.03-PICK UP RM IN, DAX7	1.2.04-BNEADROOM ENT Report Date: 3/2/2021, 1:57-42PM Eastern Standard Tim	Event Details	2/16/2021 Access Greinfed	410433578	41
On Guard 7.4 Access Granted by B On Guard 7.4 Access Granted by B	rader QUERY: START DATE; 2/1/2021 12:00:00 AM; END DATE: 1/2/2021 11:59:55 PM; READERS: DAX7-1.2:09-PKK UP RM IN, DAX7	1.2.04-BREAKROOM ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard Tin	Frent Details	2/16/2021 Access Grented 2/16/2021 Access Stented	341906144 341892282	472 463
On Run rd 7.4 Access Granted by F	mader DUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2021 11:59:59 PM; READERS: DAX7-1.2.03-PICK UP RM IN, DAX7	1.2.04-BREAKROOM ENT Separt Date: 3/2/2021 1:57:42PM Eastern Standard Tin	Event Cetalls	2/16/2021 Access Granted 2/16/2021 Access Granted	43 61 92 95 0 34 16 91 73 1	44 45
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On Guard 7.4 Acress Granted by F	THE CHERY: START DATE: 2/1/2021 12:00:00 ANG; END DATE: 3/2/2021 11:59:58 PM; READERS; DAXT-1,2:08-PICK UP RM IN, DAX	-1.2.04-BREAKROOM ENT Report Orte: 3/2/2021, 1:57:42PM Ensiem Standard Tin		2/15/2021 Access Greeted 2/18/2021 Access Greeted	341922570 486192950	47 45
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OnGuard 7.4 Access Granted by P OnGuard 7.4 Access Granted by	OLIGHY: START DATE: 2/1/2011 12:00:00 AM; ENO DATE: 1/2/2011 11:59:59 PM; READERS: DAYG-1.2.03-PICK UP RM IN, DAYG	1.2.04-DREAKROOM ENT Report Date: 3/2/2021 1:57:A2PM Eastern Standard Tin		2/16/2021 Access Granted 2/16/2021 Access Granted	241669529 232769722	50 51
	red# QUERY: START DATE: 3/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 FM; READERS: DAX7-1,2:09-PICK UP RM IN, DAX7	-1.2.04-BREAKROOM ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard Tir	e Event Detelle	2/16/2021 Access Granted	341929623	52
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March Marc	OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DI	ATE: 3/2/2021 11:59:59 PM; READERS:	(3AX7-1.2.03-PICK UP RM IN, OAX7-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Outsils	2/17/2021 Access Grented	341818757 14
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	On Guard 7.4 Access Granted by Reeder	QUERY: START DATE: 2/1/2021 12:00:00 AM; END O	ATE: 3/2/2021 11:58:58 PM; READERS:	DAX7-1.2.09-PICK UP RIM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:43PM Eastern Standard Time	Event Details	2/17/2021 Access Granted	498174401 18
Mart	OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END O	ATE: 3/2/2021 11:59:55 PM; READERS:	DAXX-12.09-PICK UP RIM IN, DAXX-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Evert Dotels	2/17/2021 Access Granted	
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March 1964 1965	On Guard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; SNII D	ATE: 1/2/2021 11:58:59 PM; READERS:	DAX7-1,2.03-PICK UP RIM IN, DAX7-1.2,04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/17/2021 Access Granted	341906666 37 435462756 38
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OnSearch 7.A. Access Generated by Baselet OLER'S STATE ORDER: 1/1/2011 120000 AM; BND ORDER:	OnGuard 7.4 Access Granted by Rea OnGuard 7.4 Access Granted by Ass	der CHERY: START DATE: 2/1/2011 12:00:00 AM; EN der QUERY: START BATE: 2/1/2011 12:00:00 AM; EN	ID DATE: 3/1/1021 11:59:59 PM; READE ID DATE: 3/2/1021 11:59:59 PM; READE	295: DAX7-1,2,09-PUCK UP RMI IN, DAX7-1,2,04-GREAKROOM ENT 595: DAX7-1,2,09-PICK UP RM IN, OAX7-1,2,04-GREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Debils	2/16/2021 Access Granted	261672745 56
Posture of A Access Granted by Reside Column 1 Access Granted by	On Guard 7.4 Access Granted by Res On Suard 7.4 Access Granted by Res	der QUERY: START DATE: 2/1/2021 12:00:00 AM; EN der QUERY: START DATE: 2/1/2021 12:00:00 AM; EN	ID DATE: 1/2/1021 11:59:59 PM; READE ID DATE: 3/2/2021 11:59:59 PM; READE	995: DAX7-1,2,03-PICK UP RM IN, DAX7-1,2,04-BREARROOM ENT PRS: DAX7-1,2,03-PICK UP RM IN, DAX7-1,2,04-BREAKRODM ENT	Report Date: 3/2/2021 1:57:42PM Fastern Standard Time	Event Details	2/18/2021 Access Granted	251308698 58
Originar 7 A Acres Greated by Random (DERY: TANT DATE: 21/12011 12:0000 AM: DRIVER: 11/12011 12:0000 AM	On-Grand 7.4 Access Granted by Res	der QUERY: START DATE: 2/1/2023 12:00:00 AM; EN	ID DATE: 3/2/2021 13:59:59 PM; READE	ERS: DAXT-1.2.03-PICK UP RIM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/1/2021 1:57;42PM Eastern Standard Time	Event Details	2/18/2021 Access Grented	436177507 60
Deficient 74 Access Grounded by Reeder CARPY: START CARPE: 2/1/2021 12:000000 AR: DND ONE: 3/1/2021 12:00000 AR: DND O	On Guard 7.4 Access Granted by Rea	der GLIERY: START DATE: 2/1/2021 \$2:00:00 AM; EN	ID DATE: 3/2/2021 11:59:59 PM; READ!	985: DAX7-1,2:08-PICK UP RM IN, DAX7-1,2:04-IREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/16/2021 Access Granted	341906666 62
Onlinear 7.4 Access described by Resident ULENY: START DATE: 2/1/2021 119929 PM; RESIDENCE DATO-1_225-PMC (PRINT), DATO-1_2-25-PMC (PRINT), DATO-1	OnGuard 7.A Access Granted by Re-	der QUERY: START DATE: 2/1/2021 12:00:00 AM; EA der QUERY: START DATE: 2/1/2021 12:00:00 AM; EA	ND DATE: 3/2/2021 11:59:59 PM; READI ND DATE: 3/2/2021 11:59:59 PM; READI	ERS: DAX7-1,2,09-PICK UP RM IN, DAX7-1,2,04-BREAKROOM ENT ERS: DAX7-1,2,03-PICK UP RM IN, DAX7-1,2,04-BREAKROOM ENT	Separt Date: 3/2/2021 157:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Eyent Details Event Details	2/18/2021 Access Granted 2/18/2021 Access Granted	43.61.87663 64 49.61.87668 65
Optionard 7.A. Account Generated by Namedar CALERYY: START DATE: 1/1/2013 12:0000 AME: END DATE: 1/1/2013 12:0000 AME: 1/1	OnGuard 7.4 Acress Granted by Rei OnGuard 7.4 Acress Granted by Rei	tder QUERY: START DATE: 2/1/2021 12:00:00 AM; EN ider QUERY: START DATE: 2/1/2021 12:00:00 AM; EN	ND DATE: 3/2/2021 11:59:59 PM; READ!	ERS: DAXT-1.2,09-PICK UP RAILIN, CIAXT-1.2,04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/18/2021 Access Granted	232746517 <i>G</i>
	OnGuard 7.4 Access Grented by Re-	dar CIUERY: START DATE: 2/1/2021 12:00;00 AM; El	ND DATE: 3/2/2021 11:59:59 PM; READ!	ERS: DAXO-1.2.09-PICK UP RM IN, DAX7-3,2,04-BREAKROOM ENT	Report Date: 8/2/2021 1:57:42PM Eastern Standard Time	Event Detells	2/18/2021 Access Granted	175916200 69

OpSpand 7.4 Access Granted by Reader OpSpand 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2011 11:59:59 PM; READERS: DADT-1,2:03-PMCK UP RM IN, DADT-1,2:04-PMEANTOON ES QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DADT-1,2:03-PMCK UP RM IN, DADT-1,2:04-READROOM ES		Event Details Event Details	2/19/2021 Access Granted 2/19/2021 Access Granted	341920040 341912991	3
OrrGuard 7.4 Acress Graphed by Reader	OURTY: START DATE: 2/1/2012 12:00:00 AM; FIND DATE: 3/2/2012 13:25:50 PM; READERS: DAYG-12.03-FINC UP RM IN, DAYG-12.03-READERS-COME I QUERY: START DATE: 2/1/2012 12:00:00 AM; FIND DATE: 3/2/2012 13:25:50 PM; READERS: DAYG-12.03-FINC UP RM IN, DAYG-12.03-READERS-COME I QUERY: START DATE: 2/1/2012 12:00:00 AM; FIND DATE: 3/2/2012 13:25:50 PM; READERS: DAYG-12.03-FINC UP RM IN, DAYG-12.03-READERS-COME I	NT Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/19/2021 Access Grented 2/19/2021 Access Grented	341890454 455174709	5
On Guard 7.4 Access Granted by Reader On Guard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAID-1:2:05-PICK UP RM IN, DAXY-1:2:00-07/00/00 M	NT Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/19/2021 Access Granted 2/19/2021 Access Granted	341890101 341892062	7
OnGuard 7.4 Access Granted by Reader UnGuard 7.4 Access Granted by Reader	CUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXY-1.2.08-PICK UP RM IN, DAXY-1.2.04-BREADROOM G CUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXY-1.2.04-PICK UP RM IN, DAXY-1.2.04-BREADROOM G	NT Report Date: 3/2/2021 157x12PM Eastern Standard Time	Event Details	2/19/2021 Access Granted	341890233	g
OnGuerd 7.4 Access Granted by Reader OnGuerd 7.4 Access Granted by Reader	QUERY; START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/1/2021 11:59:59 PM; READERS; DATG-1,2:09-PMX UP RM IN, DATG-1,2:04-IREMURD ON E QUERY; START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:59:59 PM; READERS; DATG-1,2:04-PMX UP RM IN, DATG-1,2:04-BREADERO ON E	NT Report Date: 3/2/2021 1:57:42FM Eastern Standard Time	Event Details Event Details	2/19/2021 Access Branted 2/19/2021 Access Branted	941,890388 4361,78476	10
On Guera 7.4 Access Granted by Reader On Guard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; SND DATE: 3/2/2021 11:59:59 PM; READERS: DAXT-1.2:09-PICK UP RM IN, DAXT-1.2:04-BREADROOM E QUERY: START DATE: 2/1/2021 12:00:00 AM; SND DATE: 3/2/2021 12:59:59 PM; READERS: DAXT-1.2:09-PICK UP RM IN, DAXT-1.2:04-BREADROOM E	NT Report Date: 3/2/2021 1:57:12PM Eastern Standard Time	Event Details Event Details	2/19/2021 Access Granted 2/19/2021 Access Granted	341925548 341920107	13
On Stand 7.4 Access Granted by Reader Dr.G., and 7.4 Access Granted by Seader	QUERY: START DATE: 2/1/2021 12:00:00 AM; BYD GATE: 3/2/2021 11:59:59 PM; READEUS: DAGY-12:00-PICK UP RM IN; DAXY-12:00-BREADEO GM C QUERY: START DATE: 1/1/2021 12:00:00 AM; BYD DATE: 3/2/2021 11:59:59 PM; READEMS: DAXY-12:00-PICK UP RM IN; DAXY-12:00-P	her Report Date: 3/4/2021 1:57:425*M bestern Standard Time	Event Details Event Details	2/19/2021 Access Grented 2/19/2021 Access Grented	34 1892767 34 1913079	15
OnGuerd 7.4 Access Granted by Reader OnGuerd 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:58:59 PM; READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREADROOM E QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREADROOM E	NT Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details Event Details	2/19/2021 Access Granted 2/19/2021 Access Granted	3418893CS 341922904	16 17
OnGuard 7.4 Access Granted by Render OnGuard 7.4 Access Stretted by Rander	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DADY-12:03-PICK UP RM IN, DAX7-12:04-BREADROOM E QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXY-12:03-PICK UP RM IN, DAXY-12:04-BREADROOM E	NT Report Date: 3/2/2021. 1:57:42PM Eastern Standard Time NT Report Date: 3/2/2021. 1:57:42PM Eastern Standard Time	Event Octalia Event Octalis	2/19/2021 Access Grented 2/19/2021 Access Grented	410433578 343,892569	18 19
OnGuerd 7.4 Access Smitted by Reader OnGuerd 7.4 Access Granted by Reader	QUERY; START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/2/2021 11:59:59 PM; READERS; DADY-1,2,03-PICK UP RM IN, DAX7-1,2,04-BREARROOM E QUERY; START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS; DADY-1,2,04-PICK UP RM IN, DAX7-1,2,04-BREARROOM E	NT Report Oate: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/19/2021 Access Smitted 2/19/2021 Access Grented	841897282 341912682	20
On Guard 7.4 Access Granted by Reader DoGward 2.4 Access Granted by Reader	QUERY: STANT DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READRIS: DAX7-12:01-PICK UP RIMIN, DAX7-12:01-PICK UP RIMIN RIMIN PICK UP RIMIN RIMIN RIMIN PICK UP R	NT Report Date: 3/2/2021 1:57:42PM Featern Standard Time	Event Details Event Details	1/19/2021 Access Grented 2/19/2021 Access Branted	341913365 341910396	22
DeGuard 7.4 Access Granted by Reader DeGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM: BND DATE: 3/2/2021 11:59:59 PM; READERS: DAXX-1.2:08-PICK UP RIMIN, DAXX-1.2:04-BREADERS OM E QUERY: START DATE: 2/1/2021 12:00:00 AM: DND DATE: 3/2/2021 11:59:59 PM; READERS: DAXX-1.2:09-PICK UP RIMIN, DAXX-1.2:04-BREADERS OM E		Event Details Event Datalia	2/15/2021 Access Stanted 2/18/2021 Access Granted	486174781 486188060	24 25
OnGuard 7.4 Access Granted by Render OnGuard 7.4 Access Granted by Render	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:55 PM; READERS: QAX7-1.2:09-PKX UP EM IN, DAX7-1.2:04-BREADROOM E QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2021 11:59:59 PM; READERS: DAX7-1.2:09-PKX UP RM IN, DAX7-1.2:04-BREADROOM E		Event Details Event Details	2/19/2021 Access Granted 2/19/2021 Access Granted	436177838 436178432	26 27
OnGuerd 7.4 Access Granted by Reader OnGuerd 7.4 Access Granted by Reader	CURRY: START DATE: 2/1/20112-20000 AM; END DATE: 3/2/2021 11:58:59 PM; READERS; DAX7-1,2/03-PICK UP RM N, DAX7-1,2/04-BREADEROOM E CURRY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:58:59 PM; READERS; DAX7-1,2/03-PICK UP RM IN, DAX7-1,2/04-BREADEROOM E	NT Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Brent Details Brent Details	2/15/2021 Access Granted 2/19/2021 Access Granted	343922111 410432873	28 29
On Guard 7.4 Access Granted by Reader On Guard 7.4 Access Granted by Reader	QUERY: START DATE: 1/1/2021 12:00:00 AM; END DATE: 1/2/2011 11:5959 PM; READERS: DAT-1.1.03-PICK UP RM IN, DAX-1.1.04-BREARROOM E DUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2011 11:5959 PM; READERS: DAX-1.1.03-PICK UP RM IN, DAX-1.1.04-BREARROOM E	NT Report Date: 3/2/2021, 1:57x129M Gastern Standard Time	Event Detells Event Octalls	2/19/2021 Access Granted 2/19/2021 Access Granted	341925216 341923970	30 31
Dn6yeed 7.4 Access Granted by Reader Linduard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 0AM; END DATE: 3/1/2021 11:59:59 PM; READERS: DATZ-1.2.01-FICK UF RMIN, DAZZ-1.2.01-BREADEROUSE QUERY; START DATE: 2/1/2021 12:00.00 AM; END DATE: 1/1/2021 12:00.00 PM; READERS: CADZ-1.2.01-FICK UF RMIN, DAZZ-1.2.01-BREADEROUSE CADZ-1.2.01-FICK UF RMIN, DAZZ-1.2.01-FICK UF R	NT Report Date: 3/2/2021 1:57:42PM Sestem Standard Time	Event Details Syent Details	2/19/2021 Access Granted 2/19/2021 Access Granted	941892326 241899230	37
OnGuerd 7.4 Access Granted by Reader OnGuerd 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:58:59 PM; READERS: DAXY-1_2.03-PICK UP RM IN, DAXY-1_2.03-REAGROOME QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXY-1_2.03-PICK UP RM IN, DAXY-1_2.03-REAGROOME	NT Report Date: 8/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/19/2021 Access Granted 2/19/2021 Access Granted	341925350 341890520	34 35
On Guard 7.A Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AV; END DATE: 3/2/2021 11:39:59 PM; READERS: DATE-12:00-PKX UP RM IN, DATE-12:00-RESEARCH OME QUERY: START DATE: 2/1/2021 12:00:00 AV; END DATE: 3/2/2021 11:39:59 PM; READERS: DATE-12:00-PKX UP RM IN, DATE-12:00-RESEARCH OME QUERY: START DATE: 2/1/2021 12:00:00 AV; END DATE: 3/2/2021 11:39:59 PM; READERS: DATE-12:00-PKX UP RM IN, DATE-12:00-READERS OME	NF Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details	2/19/2021 Access Granted 2/19/2021 Access Granted	341906668 486177502	36 27
On Stand 7.4 Access Greated by Reader On Stand 7.4 Access Greated by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2021 11:58:58 PM; READERS: DATF-1.2:08-PICK UP RM W, DATF-1.2:04-BREARROOM B	NT Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/15/2021 Access Granted	34 1906382 410430035	38 39
On Guard 7.4 Access Granted by Reader On Guard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2011 11:9:59 PM; READERS: CART-1.2:03-PICK UP RM IN, DAXT-1.2:04-BREADEROOM E QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:9:59 PM; READERS: DAXT-1.2:04-PICK UP RM IN, CAXT-1.2:04-BREADEROOM E	INT Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details Event Details	2/19/2021 Access Grented 2/19/2021 Access Grented	341906868	40
OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:50:59 PM; READERS: DARY-1.2.08-PCX UP RM IN, DAX7-1.2.04-BREADROOM B QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:58:59 PM; READERS: DRE7-1.2.08-PCX UP RM IN; DAX7-1.2.04-BREADROOM B	2NT Report Date: 3/2/2021 1:57:12PM System Standard Time	Event Details Event Details	2/19/2021 Access Granted 2/19/2021 Access Granted	45.5188720 341925284	41. 42
OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2021 11:58:59 PM; READERS; DADF-1,2,01-HDX UP RM IN, DADF-1,2,01-BREADEDOM E QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/3/2021 11:58:59 PM; READERS; DADF-1,2,01-PDX UP RM IN, DADF-1,2,01-BREADEDOM E	NT Report Date: 3/2/2021 157: I2PM Eastern Standard Time	Event Details Event Details	2/19/2021 Access Branted 2/19/2021 Access Grented	2175797972 261669595	44
On Guard 7.4 Access Smotted by Reader Dn Stand 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DADF-12:00-PICK UP RIM IN, DAX7-12:00-BREADERS: GADERS: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-12:00-PICK UP RIM IN, DAX7-12:00-BREADERS: DAX7-12:00-PICK UP RIM IN, DAX7-12:00-PICK UP RIM I	NT Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/19/2021 Access Granted 2/19/2021 Access Granted	341925072 341891952	45 46
On Guard 7.4 Access Granted by Reader On Guard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/201112:00:01 AM; END DATE: 3/2/20111159:59 PM; READERS: DADF-12.03-PICK LP RM (N, DACF-12.04-BREAKROOM & QUERY: START DATE: 2/1/202112:00:00 AM; END DATE: 3/2/20211159:59 PM; READERS: DAXF-12.03-PICK LP RM (N, DAXF-12.04-BREAKROOM I	:NT Report Date: 3/2/2021 1:579(3PM Eastern Standard Tires	Event Deteils Event Deteils	2/15/2021 Access Granted 2/15/2021 Access Granted	410492879 341922970	47 48
OnGuard 7.4 Access Granted by Rauder OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:50 PM; READERS; DAXT-1:2.09-PICK UP RM IM, DAXT-1:2.04-BREADROOM I QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXT-1:2.09-PICK UP RM IM, DAXT-1:2.04-BREADROOM I	INT Report Date: 3/2/2021 1:57:12PM Sestem Standard Time	Event Ortalia	2/19/2021 Access Granted 2/19/2021 Access Granted	436174731 436177838	48 50
On Suard 7,4 Access Granted by Reader On Suard 7,4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 1230000 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXT-1,2,03-PICK UP RM IN, DAXT-1,2,04-BREAKROOM I QUERY: START DATE: 2/1/2021 1230000 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXT-1,2,03-PICK UP RM IN, DAXT-1,2,04-BREAKROOM I	SMT Report Date: 3/2/2021 1:57:42PM Gastern Standard Time	Event Details Event Details	2/19/2021. Access Grented 2/19/2021. Access Grented	341906734 410629985	51 52
On Buard 7.4 Access Stanted by Reader Dri Guard 7.4 Access Stanted by Reader	QUERY; START DATE: 2/1/2021 22:00:00 AM; END DATE: 3/2/2021 11:50:50 PM; READENS: DAX7-1-2:03-PICK UP RM IN, DAX7-1-2:04-BRANNOOM! QUERY; START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:50:50 PM; READENS: DAX7-1-2:03-PICK UP RM IN, DAX7-1-2:04-BRANNOOM!	SNT Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/19/2021 Access Granted 2/19/2021 Access Granted	341918664 341890685	53 54
OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 13:00:00 AM; SNO DATE: 3/2/2021 11:59:59 PM; READERS: DAXO-1.2.03-PICK UP HM IN, DAX7-1.2.04-RREADERS: DAXO-1.2.03-PICK UP HM IN, DAX7-1.2.04-PICK UP HM IN, DAX7-1.2.03-PICK UP HM IN, DAX7-1.2.03-PICK UP H	FNT Report Date: 2/2/2021 1:57:42PM Eastern Standard Time FNT Report Date: 2/2/2021 1:57:42PM Eastern Standard Time	Event Datails Event Datails	2/19/2021 Access Granted 2/19/2021 Access Granted	251307818 2327@722	55 56
OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2011 11:59:59 PM; READERS: DAXY-12.03-PICK UP RM IN, DAXY-12.04-BREAKROOM! QUERY: START DATE: 2/1/3021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXY-12.03-PICK UP RM IN, DAXY-12.04-BREAKROOM!	INT Report Date: 3/2/2021, 1/57/42PM Eastern Standard Time	Event Detells Event Detells	2/19/2021 Access Grented 2/19/2021 Access Grented	341906470 269218731	57 58
Ondanid 7.4 Access Greated by Reader On Guard 7.4 Access Greated by Reader	QUERY; START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2021 11:59:59 PM; READERS: DAX7-1.2:09-PICK UP RM IN, DAX7-1.2:04-BYEARNOOM I QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2:03-PICK UP RM IN, DAX7-1.2:04-BYEARNOOM I	ENT Report Date: 8/1/2021 1:57:XZPM Eastern Standard Time	Event Octalis Event Octalis	2/19/2021 Access Granted 2/19/2021 Access Granted	241911449 341925923	59 60
On Guard 7.4 Access Granted by Reader Dn Guard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2021 11:59:59 PM; READERS: DAYS-12:00-PICK UP RM RV, CAXY-12:04-BREAKROCKM QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAYS-12:08-PICK UP RM IN; CAXY-12:04-BREAKROCM II	ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Octobs Event Details	2/19/2021 Access Grented 2/19/2021 Access Grented	43.5190708 341906668	67 62
On@used 7.4 Access Granted by Reader On@used 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; BND IA16: 3/2/201) 31:59:59 PM; READERS: DAX7-12.03-PICK UP RM IN, DAX7-1.2.04-BREAKROOM: DUERY: START DATE: 2/1/2031 12:00:00 AM; BND DATE: 3/2/2031 31:58:59 PM; READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREAKROOM:	ENT Report Date: 1/2/2021 1:57:42PM Eastern Standard Time	Event Detells Event Detells	2/19/2021 Access Granted 2/19/2021 Access Granted	292746517 251206841	63 64
On Guard 7.4 Access Granted by Resder On Guard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; BND DATE: 8/2/2021 11:59:59 PM; READERS: DAK7-1.2:03-PICK UP RM IN, DAX7-1.2:04-BREADROOM	ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detells Event Detells	2/19/2021 Access Granted 2/18/2021 Access Granted	4861,87669	66 68
On Guard 7.4 Access Granted by Reader On Guard 7.4 Access Granted by Reader		ENT Report Cate: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/20/2021 Access Grented 2/20/2021 Access Grented	341920040 341933387	1 2
OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Granted by Reader	QUERTY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2021 11:59:59 PM; READERS: DATY-1.2:09-PICK UP RM W, DATY-1.2:04-BREADROOM	ENT Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details Event Details	2/20/2023 Acress Grented 2/20/2021 Acress Grented	341925372 341910920	i
On@used 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:58:59 PM; READERS: DAX7-1.2:09-PICK UP RM IN, DAX7-1.2:04-BREARROOM	ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/20/2021 Access Granted 2/20/2021 Access Granted	341892062 436174709	5
On Guard 7.4 Access Stanted by Reader On Guard 7.4 Access Stanted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AA; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2.09-PICK UP RM IN, DAX7-1.2.04-BREADROOM	ENT Report Date: 3/2/2025 1:57:42PM Eastern Standard Time	Event Details Event Details	2/20/2021 Access Granted	341919556	7
On Guard 7.4 Access Granted by Reeder On Guard 7.4 Access Granted by Rander	QUERT: STATE DATE: 2/1/2021 12:00:00 AM; END DATE: 9/2/2021 11:59:59 PM; READERS: DAX7-1.2:03-PICK UP RIA IN, DAX7-1.2:04-BREAKROOM	ENT Report Oate: 3/2/2021, 1:57:42PM Eastern Standard Tines	Event Details Event Details	2/20/2021 Access Grented 2/20/2021 Access Grented	341909144 341912079	9
OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:55 PM; READERS: DAX7-1.2:03-PICK UP RM IR, DAX7-1.2:04-BREARROOM	ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard Time		2/20/2021, Access Granted 2/20/2021, Access Granted	439189060 341912991	10 11
On Guard 7.4 Access Granted by Reader On Guard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2011 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: CAX7-1.2:09-PKX UP RM IN, DAX7-1.2:04-BREAKROOM	GRVT Report Date: 3/2/7021 1,57:42PM Eastern Standard Time	Event Details	2/20/2021 Access Granted 2/20/2021 Access Granted	436174401 341920107	12
On Quard 7.4 Access Granted by Résée LinGuard 7.4 Access Granted by Resée	GUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:58:59 PM; READERS: DAX7-1,2.04-PICK UP RM IN, DAX7-1,2.04-BREAKRODIA	GNT Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details Event Details	2/20/2021 Access Branted 2/20/2021 Access Branted	436176478 426192047	14 15
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On Guard 7.4 Access Granted by Reads On Guard 7.4 Access Granted by Reads		ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Octalis	2/20/2021 Access Greated 2/20/2021 Access Greated	436174665 341519644	24 25
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On Suard 7.4 Access Granted by Reads On Guard 7.4 Access Granted by Reads	QUERY; START DATE: 2/1/2021 12:0000 AN; END DATE: 3/2/2021 11:58:59 PM; READERS: DAX7-12:08-PICK UP RM IN, DAX7-12:04-BREAKROOM QUERY: START DATE: 2/1/2021 12:00:00 AN; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-12:03-PICK UP RM IN, DAX7-12:04-BREAKROOM	ENT Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time ENT Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details Event Details	2/20/2021 Access Granted 2/20/2021 Access Granted	341892282 341925218	28 29
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OnGuard 7.4 Access Granted by Reed OnGuard 7.4 Access Streeted by Reed	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAIO-1.2.09-PICK UP RM IN, DAYO-1.2.04-BREAKROUM	ENT Report Date: 3/2/2021 1:57:42PM Seatom Standard Tim	Event Details	2/20/2021 Access Granted 2/20/2021 Access Granted	410437542 281889595	42 43
On Suard 7.4 Access Standard by Read On Suard 7.4 Access Stranded by Read	CUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXT-1.2:08-PICK UP RM IN, DAG-1.2:04-BREAKROOM	LENT Raport Date: 3/2/2021, 1:57:42PM Eastern Standard Tim	e Event Details	2/20/2021 Access Granted 2/20/2021 Access Granted	341919644	44 45
On Guard 7.4 Access Granted by Rend On Guard 7.4 Access Granted by Rend	W QUERY: START DATE: 2/1/2011 12:00:00 AM; END DATE: 3/2/2011 11:59:59 PM; READERS; DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREAKROOM	FENT Report Octor: 3/2/2023 1:57:AZPIM Eastern Standard Tim	e Event Details	2/20/2021 Access Granted 2/20/2021 Access Granted	341891731 436182950	46 47
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On Stand 7.4 Access Standard by Read On Stand 7.4 Access Standard by Read	BY CRUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2:08-PICK UP RM IN, DAX7-1.2:04-BREAKROOD	A ENT Report Date: 3/2/2021 157962PM Eastern Standard Tin	e Event Details	2/20/2021 Access Granted 2/20/2021 Access Granted 2/20/2021 Access Granted	341891027	57 58 59
On Suard 7.4 Access Granted by Read On Suard 7.4 Access Granted by Read	ler - QUERY: START DATE: 2/1/2021 12:00:001 AM; BND DATE: 3/2/2021 11:5159 PM; READERS: DAX7-1.2.03-PICK UP RAVIN, DAX7-1.2.04-BREAKRODI	A ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard Tit	e Event Details	2/20/2021 Appears Granted	251309052	80
OnGuard 7.4 Access Granted by Reas OnGuard 7.4 Access Granted by Reas	🟣 QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2021 11:58:55 PM; READERS: DAX7-1.2.03-PICK UP RIM IN, DAX7-1.2.04-BREAKROOF	A EMT Report Date: 3/2/2021 1:57:42PM Seatem Standard Tin	Event Deteils	2/21/2021. Access Granted 2/21/2021. Access Granted	289218546 341913287	1
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On Sweet 7.4 Access Standed by Resi Dir Guard 7.4 Access Wraphyd by Resi	MET QUERY: START DATE: 2/1/2021 12:00:01 AM; SHO DATE: 3/2/2021 11:50:59 PM; READERS: DAXF-1.2:09-PKX UP RM IN, DAXF-1.2:04-REAKROOL MET QUERY: START DATE; 2/1/2021 12:00:00 AM; SHO DATE: 1/2/2021 11:50:59 PM; READERS: DAXF-1.2:00-PKC UP RM IN, DAXF-1.2:04-BKEAKROOL	W ENT Report Date: 3/2/2021 1:57:42PM Eastern Standard Tir	e Event Details	2/21/2021 Access Granted 2/21/2021 Access Granted	841892052	5
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Origund 7.4 Acress Granted by Rea Origund 7.4 Acress Granted by Rea	der Query; Start Date: 2/1/2021 12:00:00 AM; END Date: 1/2/2021 11:5959 PM; READERS: DAXY-12:00-PICK UP RM IN, DAXY-12:00-BREAKROD der Query; Start (Jate: 2/1/2021 12:00:00 AM; END Date: 1/2/2021 11:5959 PM; READERS: DAXY-12:00-PICK UP RM IN, DAXY-12:00-BREAKROD	M ENT Report Cate: 3/2/2021 1:57:42PM Enstern Standard To M EN7 Report Date: 3/2/2021 1:57:42PM Eastern Standard To	ne Event Detaăs	2/21/2021 Access Grantes 2/21/2021 Access Grantes	43.6178476	13 14
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OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXT-1.2.03-PCK UP RM Nr, DAXT-1.2.04-BREAKRIDIM ENT QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXT-1.2.08-PICK UP RM NR, DAXT-1.2.04-BREAKRIDIM ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details	2/21/2021 Access Granteri	341919622 410492873	21 22
OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:0000 AM; END DATE: 9/1/2021 11:59:59 PM; READERS: DAX?-1.2.03-FICK UP RM IN; DAX?-1.2.04-BREAKROOM ENT QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/1/2021 13:95:9 PM; READERS: DAX?-1.2.03-FICK UP RM IN; DAX?-1.2.04-BREAKROOM ENT		Event Details Event Details	2/21/2021 Access Streeted 2/21/2021 Access Streeted	341892084	23
On Suard 7.4 Access Granted by Reader On Buard 7.4 Access Granted by Reader	CHERY: START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2.09-PICK UF RM IN, DAX7-1.2.09-BREAKROOM ENT	Report Data: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/21/2021 Access Granted	341925548 341925218	24 25
OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2021 11:50:50 PM; READERS: DADG-1.2.00-FICK UP RM IN, DAX7-1.2.04-RREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Facture Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/21/2021 Assess Granted 2/21/2021 Assess Granted	341885757	25
OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 1/1/2011 12:00:00 AM; END DATE: 3/2/021 11:58:59 PM; READERS: DAXT-1.2.03-PKK UP RM IN, DAXT-1.2.03-BREAKROOM ENT QUERY: START DATE: 1/1/2012 12:00:00 AM; END DATE: 1/2/2021 11:59:59 PM; READERS: DAXT-1.2.03-PKK UP RM IN, DAXT-1.2.04-BREAKROOM ENT	Report Date: 5/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/21/2021 Access Granted	341892328 341913865	27 28
On Guard 7,4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:0000 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXY-1.1.03-PICK UP RM IN, DACY-1.2.04-BREAKFOOM ENT QUERY: START DATE: 2/1/2021 12:0000 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXY-1.2.00-PICK UP RAMIN, DATY-1.2.04-BREAKFOOM ENT	Report Data: 3/2/2021 1:57:42PM Eastern Standard Time Report Data: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datella Event Datella	2/21/2021 Access Granted 2/21/2021 Access Granted	34 18925 6 8	29
OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Granted by Reader	CHERRY: START DATE: 7/1/2021 12:08:00 AM: END DATE: 3/2/2021 11:58:59 PM: READERS: DAX7-1.2:08-PICK UP RM IN, CAX7-1.2:08-BREADROCH ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/21/2021 Access Grented 2/21/2021 Access Grented	426174379 341925350	30 31
DeGuard 7.4 Access Granted by Reader	QUERY: START DATE: \$\frac{2}\frac{1}{2}\frac	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/21/2021 Access Granted	341924402	32
OnGuard 7.4 Access Granted by Render OnGuard 7.4 Access Granted by Render	DUFRY: START DATE: 2/1/2021 12:00:00 AM: \$ND DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1:2.03-PICK UP RM IM, DAX7-1:2:04-BREAKROOM ENT	Report Date: 3/2/2021 2:579/2PM Eastern Standard Time Report Date: 3/2/2021 1:57:92PM Gastern Standard Time	Event Details	2/21/2021 Access Branted 2/21/2021 Access Granted	341910986 341919644	33 34
On Suand 7.4 Access Granted by Reader On Guard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; BND DATE: 3/1/2021 11:59:59 PM; READERS: DAXT-1.2:03-PICK UP RM IN; DAXT-1.2:04-BREAKROOM ENT CHUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:59:50 PM; READERS: DAXT-1.2:03-PICK UP RM IN; DAXT-1.2:04-BREAKROOM ENT	Report Date: 3/2/2021 157/42PM Eastern Stendard Time	Event Details	2/21/2021 Access Grented	341864594	35
DeGuard 7.4 Access Granted by Reader	DIRECTOR TO ATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11:5R:59 PM; READERS; DAX7-1.2:09-PICK UP RM:N, DAX7-1.2:09-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Cotalls	2/21/2021 Access Granted 2/21/2021 Access Granted	341926738 289219546	26 37
OnGuerd 7.4 Access Grented by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/1/2021 11:00:59 PM; READERS: DADY-12:03-PICK UP RM IN, DADY-12:04-BREAKROOM ENT QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:09:59 PM; READERS: DADY-12:03-PICK UP RM IN, DADY-12:04-BREAKROOM ENT	Report Date: 3/2/2021 2:57:42PM Eastern Standard Time	Event Details	2/21/2021 Access Granted	541925284	38
OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Branted by Reader	CHIERY: START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2,U3-PICK UP RM IN, DAX7-1,Z-04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detells	2/21/2021 Access Brented 2/21/2021 Access Brented	341869306 436163060	59 43
OnGuard 7.4 Access Granted by Reader DinGuard 7.4 Access Granted by Rander	QUERY: START DATE: 2/1/2021 12:0000 AM; END DATE: 9/2/2021 11:59:59 PM; READERS: DAYZ-12:05-PICK UP RM (N, DARZ-12:06-9REAKROOM ENT DUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2021 11:59:55 PM; READERS: DAYZ-12:05-PICK UP RM (N, DAYZ-12:04-BREAKROOM ENT	Report Date: a/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/21/2021 Access Granted	436192950	41 42
OnGuero 7.4 Access Grented by Reader	CHERY- START DATE: 1/1/2021 12-00:00 AM: END DATE: 1/2/2021 11:5859 PM: READERS: DAX7-1.2.09-PICK LIF RM:N, DAX7-1.2.04-BREAKRIDOM ENT	Report Date: 3/2/2021 1:57:42PM Fastern Standard Time Report Date: 1/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/21/2021 Access Granted 2/21/2021 Access Granted	289219546 410430081	45
On Buard 7.4 Access Granted by Reader On Guard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/1/3021 11:50:50 PM; READERS: DATO-12:03-7/CK UP RM IN, DATY-12:04-BREAKROOM ENT CUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 8/2/3021 11:50:50 PM; READERS: DATY-12:02-PKX UP RM IN, DAKY-12:04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:12PM Eastern Standard Time	Event Datalis	2/21/2021 Access Granted 2/21/2021 Access Stanted	161669595 172905979	44 145
Op@uard 7.4 Access Granted by Reader	QUERY: START DATE: 271/2021 12:0000 AM; END DATE: 471/2021 11:30:30 PM; READERS: DAXY-1-2.01-PICK UP RM IV, DAXY-1-2.01-BREAKROOM ENT QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/1021 11:30:50 PM; READERS: DAXY-1-2.03-PICK UP RM IN, DAXY-1-2.01-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Datails	2/21/2021 Access Granted	410431727	46
OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Granted by Reader	OHERY: START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11:59:59 PM: READERS: DAX7-1.2:00-PICK UP RM IN, 0x07-1.2:04-BREARDOM ENT	Report Date: 3/2/2021, 1:57:42PM Fasture Standard Time	Event Details Event Details	2/21/2021 Access Granted 2/21/2021 Access Granted	341891731 410431727	47 43
Conferent 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2011 19:00:00 AM; FIND DATE: 3/2/2011 15:05:0 PM; READERS: DAY:-1.2.08-PICK 19 RM:N, PNOT-1.2.04-RMSAXROOM ENT QUERY; START DATE: 2/1/2011 12:00:00 AM; END DATE: 3/2/2011 13:00:00 PM; READERS: DAY:-1.2.03-PICK UP RM:NI, DAY:-1.2.04-RMSAXROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57.42PM Eastern Standard Time	Event DateSe	2/21/2021 Access Stanted	436593950	49
OnGuard 7.A Access Granted by Reader OnGuard 7.A Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; BND DATE: 3/2/2021 11:58:59 PM; READERS; DAXZ-1.2.02-PKX UP RM PK, DAXZ-1.2.04-BILEAKROOM ENT	Report Date: 3/2/2021 1:57x12PM Seatern Standard Time Report Date: 3/2/2021 1:57x42PM Seatern Standard Time	Event Datalis Event Datalis	2/21/2021 Access Granted 2/21/2021 Access Granted	341892084 410492873	50 51
On Guard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 9/2/2021 11:9559 PM; READERS: DAXY-1.2.03-PKZ UP RM IN, DAXY-1.2.01-PKE	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/21/2021 Access Granted	342888757	52
On Guard 7.4 Access Granted by Reader On Stantal T.4 Access Granted by Reader	QUERY; START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2:09-91CK LIP RM IN, CAX7-1.2:04-BREAKROOM ENT	Report Oate: 1/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detells	2/21/2021 Accuss Grented 2/21/2021 Accuss Stanted	24190GI82 426192950	53 54
OnGuard 7.4 Access Stranted by Reader OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2.03-PCK UP RM IN, DAX7-1.2.04-BREAKROOM ENT QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 13:59:59 PM; READERS: DAX7-1.2.03-PCK UP RM IN, DAX7-1.2.04-BREAKROOM ENT	Report Data: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datalla	2/11/2021 Access Granted	134591775	55
OnGuard 7.4 Access Granted by Reader	DIRECT: START CATE: 3/1/2021 12 COOD AM: PND CATE: 3/1/2021 12-59:59 PM: READERS: DAX7-1.2.03-PKX UP RM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Svent Details Event Details	2/21/2021 Access Granted 2/21/2021 Access Granted	341919864 430490021	56 57
On Guard 7.4 Access Granted by Reader On Guard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:95:59 PM; READERS: DAXT-1.2.03-PICK UP RM IN, DAXT-1.2.03-RISKARDOM ENT QUERY: START DATE: 2/1/2021 12:00.00 AM; END DATE: 3/1/2021 11:56:55 PM; READERS: DAXT-1.2.03-PICK UP RM IN, DAXT-1.2.03-REARROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Octalis	2/21/2021 Access Granted 2/21/2021 Access Stanted	251334735 341911493	58 59
OnGuard 7.4 Appess Grented by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:06:00 PM; READERS: DAX7-1.2:06-PICK UP RM IN, DAX7-1.2:04-RREAKROOM ENT QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/1/2021 12:00:00 PM; READERS: DAX7-1.2:08-PICK UP RM IN, DAX7-1.2:04-RREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Datails	2/21/2021 Access Granted	341908822	60
OrrGuard 7.4 Access Granted by Reader OrrGuard 7.4 Access Granted by Reader	OHERY: START DATE: 2/1/2021 12:00:00 AM: ERD DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2.03-FICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Seatern Standard Time	Event Datalis Event Datalis	2/21/2021 Access Branted 2/21/2021 Access Granted	341908470 261572745	62 ET
On Suand 7.4 Access Grammed by Reeder	QUERY: START DATE: 2/1/2021 12:0000 AM; END DATE: 3/2/2021 11:9:89 PM; READERS: DADY-1.2:03-PKK UP RM IN, DADY-1.2:04-88EARRDOM ENT QUERY: START DATE: 2/1/2021 12:0000 AM; END DATE: 3/1/2021 11:5:8:58 PM; READERS: DAXY-1.2:03-PKC UP RM IN, DAXY-1.2:04-88EARRDOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 2:57:42PM Eastern Standard Time	Event Detells	2/21/2071 Access Granted	34.1927090	63
On@sard 7.4 Access Granted by Reads On@sard 7.4 Access Granted by Reads	QUERY; START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:58:59 PM; REPORTS: DAX7-1-2:09-PICK UP RM:1N, DAX7-1-2:01-BREAKR/JOMEN!	Report Date: 3/2/2021 1:575(2PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/21/2021 Access Graphed 2/21/2021 Access Graphed	341925929 218611830	64 66
On Suard 7.4 Access Granted by Reader On Guard 7.4 Access Granted by Reade	OLICEY, CTART DATE: 2/5/2021 12:00:02 AM: PRO DATE: 3/2/2021 11:59:59 PM: READERS: DAX/-1.203-PUX UP HM IN, DAX/-1.203-9/2021 AM: PRO DATE: 3/2/2021 11:59:59 PM: READERS: DAX/-1.203-PUX UP HM IN, DAX/-1.203-9/2021 AM: PRO DATE: 3/2/2021 11:59:59 PM: READERS: DAX/-1.203-PUX UP HM IN, DAX/-1.203-9/2021 AM: PRO DATE: 3/2/2021 11:59:59 PM: READERS: DAX/-1.203-PUX UP HM IN, DAX/-1.203-9/2021 AM: PRO DATE: 3/2/2021	Report Date: 3/2/2021 1:57:42PM Fastern Standard Time	Event Details	2/21/2021 Access Granted	341906868	66
On Guard 7.4 Access Stramad by Reede	OLIERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:58:58 PM; READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 8/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/21/2021 Access Granted 2/21/2021 Access Granted	289219546 341926738	67 68
OnGuard 7.4 Access Granted by Reads OnGuard 7.4 Access Granted by Reads	CLIERY: START DATE: 2/1/2031 12:00:00 AM; GND DATE: 1/2/2021 11:58:59 PM; READENS: DAX7-1-2,04-PICK UP RM IN, DAX7-1-2,04-BREAKROOM ENT	Report Cate: 3/2/2021 1:57:42PM Eastern Standard Time	Event Cetrils	2/21/2021 Azzeza Granted	7519(19052	69
On Suand 7.4 Access Smitted by Reade	MINNY START DATE: 1/1/3021 13:00:00 AM: END DATE: 3/2/2021 11:59:59 PM: READERS: DAX7-1.2:09-PICK UP RM IN, DAX7-1.2:04-BREAKROOM ENT	Report Date: 3/2/2021 2:57:42PM Sestem Standard Time Report Date: 3/2/2021 1:57:42PM Sestem Standard Time	Event Details Event Details	2/22/2021 Access Scanted 2/22/2021 Access Scanted	341891665 436174753	2
OnGuard 7.4 Access Strented by Reads OnGuard 7.4 Access Granted by Reads	OUERY: START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11:59:59 PM; READERS: DAXO-1.2.09-FICK UP RM IN, DAXX-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detells	2/22/2021 Access Granted	410429965	3
OnGuard 7.4 Access Granted by Reads	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DACT-1.2:03-PKX UP RM IN, DACT-1.2:04-BREARROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/22/202). Access Granted 2/22/2021. Access Granted	436177793 841920040	5
On Suard 7.4 Access Granted by Reads On Suard 7.4 Access Stanted by Reads	O 16/89 - START DATE: 2/1/2021 12:1800 AM: BND DATE: 3/2/2021 11:59:59 PM; READERS: DAXY-1.2.05-PICK UP KM IN, DAXY-1.2.05-READERS DAXY-1.2.05-PICK UP KM IN, DAXY-1.2.05-READERS	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detells Event Detells	2/22/2021 Access Granted 2/22/2021 Access Granted	341971603 341890454	8 7
On/Guard 7.A Access Granted by Reads	CUJERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 8/2/2021 11:59:59 PM; READERS: DAX7-1.2:09-PCK UF RM IN, DAX7-1.2:04-BREAKROON ENT	Report Date: 2/2/2021 1:57:42FM Sestem Standard Time Report Date: 2/2/2021 1:57:42FM Eastern Standard Time	Event Details	2/22/2021 Acords Granted	241892062	í
OnGuard 7.4 Access Granted by Reads On Saurel 7.4 Access Granted by Reads	AMERY, START DATE: 2/1/2021 12:00:00 AM: END DATE: 2/2/2021 11:59:59 PM: READERS: DAUG-1, 2,03-PKX, UP RM IN, DAUG-1, 2,04-REARROOM ENT	Report Date: 3/2/2021 1:57:42PM Restart Standard Time	Event Details Event Details	2/21/202). Access Granted 2/21/2021. Access Granted	341925372	9 10
On Buard 7.4 Access Stranted by Reads	 QUERT: START DATE: 2/1/2021 12:00:00 ANA; THID DATE: 3/2/2021 11:58:59 PM; READERS: DAXT-1.2.03-PICK UP RM IN, DAXT-1.2.04-GREANBOOM ENT 	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details	2/22/2021 Access Granted	436192047	11.
OnGuard 7.4 Access Stanted by Reed OnGuard 7.4 Access Granted by Reed	 CLUERY: START DATE: 2/1/2021 12:00:00 AM: END DATE: R/2/2021 11:59:59 PM; READERS: DAX7-1.2:03-PICK UP RM IN, DAX7-1.2:04-BREAKROOM ENT 	Report Date: 2/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/22/2021 Access Granted 2/22/2021 Access Granted	341919556 341890233	12 13
On Suard 7.4 Access Granted by Read On Suard 7.4 Access Granted by Read	 DUERY: STAKY DATE: 2/1/2011 12:00:00 AM; END DATE: 1/2/2021 11:59:59 PM; READERS: DADT-1.2.09-PICK UP RM IM, DATT-1.2.04-BREAKROOM ENT 	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Cotails	2/22/2021 Access Granted	341512991 436174401	14 15
OnGuard 7.4 Access Granted by Read	" QUERY: START DATE: 1/1/2021 12:00:00 AM; END DATE: 1/1/2021 11:5959 FM; READERS: DAX7-1.2:01-PICK UP RM IN, DAX7-1.2:04-BEAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Sestem Standard Time	Event Details	2/22/2021 Azzens Grantad 2/22/2021 Azzens Grantad	430438578	16
OnGuard 7.4 Access Granted by Read OnGuard 7.4 Access Granted by Read	" CAMERY: START DATE: 7/1/2021 12:00:00 AM: ENG DATE: 8/2/2021 11:59:59 PM; READERS: DAX7-1.2:09-PICK UP RIM IN, DAX7-1.2:09-BILEAGROUNI EN	Report Date: 3/2/2021 1:57:42FM Eastern Standard Time	Event Details Event Details	2/22/2021 Access Granted 2/22/2021 Access Granted	341891D59 436175458	17 18
UnGuard 7.4 Access Granted by Read	TOURRY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXO-12:03-FICK UP RIM RY, DAXO-12:04-FICK UP	Report Date: 3/2/2021 1579/2PM Eastern Standard Time Report Date: 3/2/2021 1579/2PM Eastern Standard Time	Event Debils	2/22/2021 Access Granted	251907618	19
OnGuard 7.4 Access Granted by Read OnGuard 7.4 Access Granted by Read	CHERY START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11:59:59 FM; READERS: DAXT-1.2.03-PICK UP RM IN, DAXT-1.2.04-BREAKBOOM ENT	Report Date: 1/2/2021 1:57912PM Existent Standard Time Report Date: 3/2/2021 1:57942PM Gastern Standard Time	Event Details Event Details	2/22/2021 Access Sminted 2/22/2021 Access Grented	456178476 341924402	20 21
On Sunrid 7.4 Access Granted by Read On Sunrid 7.4 Access Granted by Read	WINDOWS START DATE: 2/1/2011 12:00:00 AM: END DATE: 3/2/2021 12:59:59 PM: READERS: DAX7-1,2,08-91CK UP RM IN, DAX7-1,2,04-8KEAKROOM ENT	Report Date: 3/2/2021 157:42PM Eastern Standard Time	Event Details	2/22/2021. Access Granted	495174731	22 23
OnGuard 7.4 Access Branted by Reed	IT DUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DATG-1.2.09-PKK UP RIM IN, DATG-1.2.09-BEARRIDOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/22/2021. Access Granted 2/22/2021. Access Granted	341919844	24
OnGuard 7.4 Access Granted by Read OnGuard 7.4 Access Granted by Read	OLERY: START DATE: 2/1/2021 12:00:00 AM: BND DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREADERGOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/22/2021 Access Grented 2/22/2021 Access Grented	410432879 436174709	25 26
On Guard 7.4 Access Granted by Read	OURRY: START DATE: 2/1/2021 12:00:00 AM; ENI DATE: 3/2/2011 11:50:59 PM; READERS: DATE-1.2:08-PICK UP RIM IN, DAYE-1.2:08-PICK UP RIM IN, DAYE	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/22/2021. Access Grented	341,891523	23
OnGuard 7.4 Access Granted by Read OnGuard 7.4 Access Granted by Read	CLIFRY: START DATE: 2/1/2021 12:50:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAUG-1,2.09-PICK UP RM IN, DAK7-1,2.09-BICKUP RM IN, DAK7-1,2.	Report Date: 8/2/2021 1:57:42PM Eastern Standard Time Report Date: 8/2/2021 1:57:42PM Eastern Standard Time	Event Datelle Event Detelle	2/22/2021. Acoust Grented 2/22/2021. Acoust Grented	241892064 241819622	28 29
OnGuard 7.4 Access Granted by Reso		Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details	2/22/2021 Acress Grented	43.6174865	30
OnGuard 7.A. Access Granted by Reed	OURRY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:50:59 PM; READERS; DAX7-1.2.03-PM; RIADERS; RI	Report Date: 3/2/2021 1:57:42PM Festern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/22/2021 Access Granted 2/22/2021 Access Granted	341912682 341890HBA	31 32
On Suard 7.4 Access Granted by Rese On Suard 7.4 Access Granted by Rese	OLERY: STANT DATE: 2/1/2001 12:00:001 AM: PRID DATE: 1/2/2001 11:59:59 PM; READERS: DADZ-1.2.08-PKX UP RM IN, DAXZ-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 3:57:42PM Eastern Standard Time	Event Details	2/22/2021. Access Granted 2/22/2021. Access Granted	341925218 341922111	33 34
On Buard 7.4 Access Grented by Real	INF QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:50:50 PM; READERS: DAX7-1,2.03-PICK UP RW/IN, DAX7-1,2.03-BREAMROOM ENT	Report Date: 3/2/2021 1:57:62PM Eastern Standard Time Seport Date: 3/2/2021 1:57:62PM Eastern Standard Time	Event Detells Brant Details	2/12/2021 Access Granted	341590520	16
OnGuerd 7.4 Access Granted by Rev OnGuerd 7.4 Access Granted by Rev	CHERY, START DATE: 2/1/20/112:00:00 AM: END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-12.03-PICK UP RIM IN, DAX7-12.03-BREADERS: DAX7-12.03-PICK	Report Date: 3/1/2021, 1:57:42PM Fastern Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details Event Details	2/22/2021 Access Granted 2/22/2021 Access Granted	241910996 436174379	36 37
On Guerd 7.4 Access Granted by Rea	FOR QUERY: START DATE: 2/1/2021 12:00:00 AM; SHD DATE: 1/2/2021 11:50:59 FM; READERS: DADY-L2.09-FM; IN, DRZY-L2.09-SHDARDOUM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datalia	2/22/2021 Access Granted	341925350	36
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ConGuard 7.4 Access Granted by Ra	THE CHEMY: START DATE: 2/1/20(1 12:00:00 AM; END DATE: 3/2/20(1 11:00:00 PM; READERS: DAV7-1.2.03-PHCK UP REATIN, DAX7-1.2.04-BREAKINGOM ENT	Report Clate: 3/2/2021 1:57:42PM Eastern Standard Time Report Clate: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details		341925923	53 54
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On Guard 7.4 Access Granted by Reader On Guard 7.4 Access Granted by Reader	QUERY; START DAYE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:50:59 PM;	READERS: DAXY-1.2.03-PICK UP RM IN, DAXY-1.2.09-BREADRIDOM ENT READERS: DAXY-1.2.03-PICK UP RM IN, DAXX-1.2.00-BREADRIDOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/23/2021 Access Granted 2/23/2021 Access Granted	218611830 436180705	45 50
OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM;	READERS: DAX7-1.2.0S-PICK UP RM IN, DAE7-1.2.0H-BREAKROOM EN	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:43PM Eastern Standard Time	Event Details Event Details	2/23/2021 Access Granted	410429943	51
On Guard 7.4 Access Granted by Reader On Guard 7.4 Access Granted by Reader	CHEWY START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM;	READERS: DAX7-1.2.03-PICK UP HM IN, DAX7-1.2.01-BREAKROOM ENT	Report Date: 3/2/2021 1:57/42PM Eastern Standard Time Report Date: 3/2/2021 1:57/42PM Eastern Standard Time	Event Datalis Event Datalis	2/23/2021 Access Granted 2/23/2021 Access Granted	341891027 289219546	52 55
On Sweet 7.4 Access Strented by Reader On Guard 7.4 Access Granted by Reader	QUERY: START DATE: 3/1/2021 12:00:00 AM; 9KD DATE: 3/1/2021 11:59:59 PM; QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM;	READERS: DAX7-1,2,119-PICK UP RM IN, DAX7-1,2,04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/23/2021. Access Grented 2/25/2021. Access Granted	252309052	55
OrrGuard 7.4 Access Granted by Render	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM;	READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:5/:/kgr Ne éastern Standard Time	Event Details	2/24/2021 Access Cranted	1534660/3	1
On Guard 7.4 Access Granted by Reader On Guard 7.4 Access Granted by Reader	CHERRY, START DATE: 2/1/2021 12:00:00 AM: WID DATE: 3/2/2021 11:59:59 PM:	READERS: DAX7-1.2.09-PICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Fastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/24/2021 Access Scanled 2/24/2021 Access Scanled	341891665 341890454	3
OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; BND DATE: 3/2/2021 11:59:59 PM; QUERY: START DATE: 2/1/2021 12:00:00 AM; BND DATE: 3/2/2021 11:59:59 PM;	READERS: DAXT-1,2.03-PLCK LIP RM IN, DAXT-1,2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/24/2021 Access Granted 2/24/2021 Access Granted	217897972 241920107	4 6
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OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Granted by Reader	OURSEY: 67497 DATE: 7/1/7021 12:00:00 AM: END DATE: 3/2/2021 11:59:59 PM:	READERS: DAX7-1.2.06-PICK UP HIM IN, GAX7-1.2.04-BREADERSHIPM ENT	Report Date: 3/2/2021 1:57:43PM Eastern Standard Tires Report Date: 3/2/2021 1:57:42PM Eastern Standard Tires	Event Details Event Details	2/24/2021 Access Granted 2/24/2021 Access Granted	341919556 341890232	7 5
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On Suard 7.4 Access Granted by Reader UnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:58:59 PM; QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM;	READERS: DAX7-1,2,02-PICK UP RM IN, DAX7-1,2,04-BREAKROOM ENT	Report Date: 3/2/2021 2:57:42PM Eastern Standard Time	Event Details	2/24/2021 Access Granted 2/24/2021 Access Smithed	241924336 436174401	11 12
OnGuerd 7.A Access Granted by Resder	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/2/2021 11:59:55 PM; QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2021 11:59:59 PM;	READERS: DAXY-1.2.CS-PICK UP RM IN, GAXY-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Streederd Tires Report Date: 3/2/2021 1:57:42PM Eastern Standard Tires	Event Details	2/24/2021 Access Granted	341913079	13
OnGuard 7.4 Access Scentad by Reader OnGuard 7.4 Access Strented by Reader	OU DOW: START DATE: 2/1/2021 12:00:00 AM: BND DATE: 3/1/2021 11:59:59 PM:	READERS: DAX7-1.2.08-PICK UP RIM IN, DAX7-1.2.04-BREAKROOM EN I	Report Date: 3/2/2021 1:57:42PM Sestern Standard Time Report Date: 3/2/2021 1:57:42PM Sestern Standard Time	Event Details Event Details	2/24/2021 Access Granted 2/24/2021 Access Granted	343824402 341913431	14 15
On Stand 7.4 Access Granted by Reader On Guard 7.4 Access Granted by Reader	QUENT: START DATE: 2/1/2011 12:00:00 AM; END DATE: 9/2/2021 11:59:59 PM; QUENT: START DATE: 2/1/2021 12:00:00 AM; END DATE: 9/2/2021 11:59:59 PM;	READERS: DAX?-1,2,03-PICK UP RM IN, DAX?-1,2,01-BREAKKOOM EMI	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datels	2/24/2021 Access Grented 2/24/2021 Access Grented	341919644 341922904	16 17
Orr@used 2.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/1/2021 11:5959 PM; QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/2/2021 11:5959 PM;	REACIERS: DAX7-1.2.03-PKX UP HM IN, DAX7-1.2.04-PREAGRICUM EN	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details Event Details	2/24/2021 Access Granted	341913189	18
On Suerd 7.4 Access Granted by Reader On Burnel 7.4 Access Granted by Reader	OURDRY START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11:59:59 PM:	READERS: DAXX-1.2.08-PICK UP RM IN, DAXX-1.2.08-BREAKROOM EN I	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/24/202). Access Granted 2/24/2021. Access Granted	436174665 341892767	70 18
OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/202112:00:00 AM; END DATE: 3/2/202111:59:59 PM; QUERY: START DATE: 2/1/202112:00:00 AM; END DATE: 3/2/202111:59:59 PM;	READERS: DAX7-1.2.03-PICK UP RM (N, DAX7-1.2.04-BREAD/DOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Outsits Event Outsits	2/24/2021 Access Granted 2/24/2021 Access Stanled	341693533	21. 22.
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OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 PAR; END DATE: 3/2/2021 11:59:59 PIA; QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2021 11:59:59 PM;	READERS: DAKT-1,2,03-PICK UP IUM IN, DAKT-1,2,04-BREAKROOM EN I	Report Date: 3/2/2021 1:57:42PM Estiern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datails Event Datails	2/24/2021 Access Granted 2/24/2021 Access Granted	341892960	24 25
OnGuard 7.4 Access Granted by Reside OnGuard 7.4 Access Granted by Reside	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM;	READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:43PM Seatern Standard Time Report Date: 2/2/2021 1:57:43PM Featern Standard Time	Event Details Event Details	2/24/2021 Access Grented 2/24/2021 Access Grented	341925548 341892414	26 27
On Suand 7.4 Access Granted by Reads On Suand 7.4 Access Granted by Reads	QUERY: START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11:59:59 PM:	READERS: DAX7-1,2,03-PICK UP RM IN, DAX7-1,2,04-BREAKROOM ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Everyt Details Everyt Details	2/24/2021 Access Granted 2/24/2021 Access Granted	436174379 341890888	28 29
On Grand 7.4 Access Scanted by Reade On Guard 7.4 Access Scanted by Reade	QUESY; START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/2/2021 11:59:59 PM;	READERS: DAXY-1.2.08-PICK UP RM IN, DAXY-1.2.04-BREARROOM ENT READERS: DAXY-1.2.08-PICK UP RM IN, DAXY-1.2.04-BREARROOM ENT	Report Date: 1/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/24/2021 Access Granted	345919622	30
On Guard 7.4 Access Granted by Reads	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM;	READERS: DAX7-1, 2.03-PICE UP RM IN, DAX7-1.2.04-DREADERS AND ENT	Report Date: 3/2/2021 1:57:43PM Seatem Standard Time Report Date: 3/2/2021 1:57:43PM Eastern Standard Time	Event Detells	2/24/2021 Access Grented 2/24/2021 Access Stended	341897326 289219546	31 32
OnGuard 7.4 Access Granted by Reads OnGuard 7.4 Access Granted by Reads	OUGHY: STAST DATE: 2/1/2021 12:00:00 AM: BND DATE: 3/1/2021 11:59:55 PM:	READERS: DAX7-1.2.03-PICK UP HIM IN, DAX7-1.2.04-BREADEROWNER)	Report Date: 3/2/2021 1:57:A3PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/24/2021 Access Grantes 2/24/2021 Access Granted	341890520 341524226	33 34
On@uard 7.4 Access Granted by Reads On@uard 7.4 Access Granted by Reads	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM;	READERS: DAX7-1,2:08-91CK LIP RM IN, DAX7-3-2.09-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Titre:	Event Datalis	2/24/2021 Access Greeted	343888757 436193060	35 36
On@uard 7.4 Access Granted by Reads	QUERY: START DATE: 2/1/2021 12:00:00 AM; SHD DATE: 3/2/2021 11:59:59 FM QUERY: START DATE: 2/1/2021 12:00:00 AM; SHD DATE: 3/2/2021 11:59:59 FM	READERS: DAX7-1, 2,03-PICK UP RM IN, DAX7-1, 2,04-BREAKROOM ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Prent Dutells Event Dutells	2/24/2021 Access Granted 2/24/2021 Access Granted	241801731	37
OnGuard 7.4 Access Granted by Reads OnGuard 7.4 Access Stanted by Reads	QUERY: START DATE: 2/3/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM	: READERS: DAXT-1.2.03-PICK UP RM IN, DAXT-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/24/2021 Access Granted 2/24/2021 Access Granted	41049357a 436192950	3B 39
On Suand 7.4 Access Smitted by Reads On Suand 7.4 Accests Smitted by Reads		: READERS: DAXT-1.2.09-PICK UP RM IN, DAXT-1.2.09-BREAKEDOM EN	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/34/2021 Access Granted 2/24/2021 Access Granted	289210546 34190668	40 41
OnGuard 7.4 Access Granted by Reads OnGuard 7.4 Access Granted by Reads	QUERY: STANT DATE: 2/1/2021 12:00:00 AAA; END DATE: 3/2/2021 12:59:59 PM	: READERS: DAXY-1,2,US-PICK UP RM IN, DAXY-1,2,04-BREAKROOM ENT : READERS: DAXY-1,2,03-PICK UP RM IN, CAXY-1,2,04-BREAKROOM ENT	Report Date: 3/2/2021, 1:57:02PM Eastern Standard Time Report Date: 1/2/2021, 1:57:42PM Eastern Standard Time	Event Details	2/24/2021. Access Streeted	341906670	42
On Guard 7.4 Access Granted by Read	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM	READERS: DAXY-1.2.03-PICK UP RM (N, DAXY-1.2.04-BREARRENIM EN I	Report Date: 3/2/2021, 1:57:4/2PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Sestem Standard Time	Event Detells Event Detells	2/24/2021 Access Brented 2/24/2021 Access Granted	43 61 88 658 34 19 133 E7	43 44
On Guard 7.4 Access Grented by Reeds On Guard 7.4 Access Grented by Reed	 DESERVE START DATE: 3/1/2021 12:00:00 AM: BND DATE: 3/2/2021 11:59:59 PM 	READERS: DAGY-1.Z.08-PICK UP RM IN, DAGY-1.Z.08-SIREAGROOM ENV	Report Date: 3/1/2021, 1:57:42PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details Event Details	2/24/2021 Access Granted 2/24/2021 Access Granted	177905979 541925972	45 48
On Guard 7.4 Access Grented by Read On Guard 7.4 Access Granted by Read	 CHECKY START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11:59:59 PM 	; READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREASROOM ENT	Report Date: 3/2/2021 157:42PM Eastern Standard Time	Event Details	2/24/2021: Access Granted 2/24/2021: Access Granted	843881952 241922970	47 48
OnGuard 7.A Access Granted by Read	QUERY: START DATE: 2/1/2021 12:00(ID AM; END DATE: 3/2/2021 11:58:59 PM	I; READERS: DAX7-1.2.03-PICK UP RIM IN, DAX7-1.2.04-BIBARRIDOM ENT	Report Date: 3/1/2021 1:57:42PM Gestern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/24/2021 Access Granted	410433578	49
On Guerd 7.4 Access Stanfed by Read On Guerd 7.4 Access Granted by Read	- OLEGOV, STADT DATE: 3/1/2001 12:00:00 AM: FRID DATE: 3/2/2001 11:59:59 PM	I: READERS: DAX7-1,2.09-PICK UP RM IN, DAX7-1,2.04-BREADRIDOK ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/24/2021 Access Smithed 2/24/2021 Access Grented	341892084 841891731	50 51
On Buard 7.4 Access Granted by Read Op Guard 7.4 Access Granted by Read	OF STATE PARTY NAME: 2017/2001 12:00:00 AM: FND DATE: 3/2/2001 11:59:59 PM	(: AFADERS: DAXY-1.2.03-PICK UP RM IN, DAXY-1.2.04-BREAEROOM EXT	Report Date: 3/2/2021 2:57:42*M Sestem Standard Time Report Date: 3/2/2021 2:57:42*M Sestem Standard Time	Event Octalis Event Details	2/24/2021 Access Granted 2/24/2021 Access Granted	251300655 426180090	52 53
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On Guard 7.4 Access Granted by Reso	OUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2021 11:59:59 PM	4: READERS: DAX7-1,2,03-PICK UP RM IN, DAX7-1,2,04-BREADHOUN ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/24/2021 Access Simulated 2/24/2021 Access Granted	25 1908683	58
On Guard 7.4 Access Granted by Read On Buard 7.4 Access Granted by Read	OF IDDAY START DATE: 2/1/2021 12:00:00 6M: END DATE: 2/2/2021 11:59:59 PM	A; READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-3.2.06-BREADERSOOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/24/2021 Access Granted 2/24/2021 Access Granted	410429965	57 38
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OnGustd 7.4 Access Granted by Resi	er OUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PA	A: READERS: DAXY-1.2.03-PICK UP RIM IN, DAXY-1.2.04-BREAGROOM ENT	Report Date: 3/2/2021 1:57:42PM Emison Standard Time Report Date: 3/2/2021 1:57:42PM Enstern Staticlard Time	Event Details Event Details	2/24/2021. Access Branted 2/24/2021. Access Granted	241906822	GI.
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On@uard 7.4 Access Snapted by Real On@uard 7.4 Access Strented by Real	OURSEL START DATE: 3/1/2021 12/00/00 AMF END DATE: 3/1/2021 12:59:50 PA	A: READERS: DAX7-1_2.09-PICK UP RM IN, CAX7-1_2,04-SREARHOUM EN I	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Outsilo		251384981 410440077	64 66
On Guard 7.4 Access Granted by Ret-	or QUERY: START DATE: 1/1/2021 12:00:00 AM; END DATE: 1/2/2021 11:50:50 P	W; READERS: DAX7-1.2/03-PICK UP RWIN, DAX7-1.2/04-BREAKROOM ENT IA: READERS: DAX7-1.2/03-PICK UP RWIN, DAX7-1.2/04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Seatern Standard Time Report Date: 3/2/2021 1:57:42PM Seatern Standard Time	Event Details	2/34/2021 Access Granted	218611830	86
OnGuerd 7.4 Access Granted by Rea OnGuerd 7.4 Access Granted by Rea	- PAISER, CTART DATE: 2/1/2021 12:00:00 AM: FAID DATE: 3/2/2021 11:59:59 PI	U: NEADERS: DAX7-1,2,03-PICK UP RM IN, DAX7-1,2,04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/24/2021 Access Granted 2/24/2021 Access Scapted	341908734 289219548	67 68
On Guerd 7.4 Access Granted by Res On Guerd 7.4 Access Granted by Res	OURDAY, START DATE: 3/1/2021 12:00:00 AM: FND DATE: 3/2/2021 11:55:59 P	M: READERS: DAXY-1.2.03-PKX UP RM IN, DAXY-1.2.04-IRR-ABROUM ENT	Report Date: 3/2/2021 L57:42PM Eastern Standard Time Report Date: 3/2/2021 L57:42PM Eastern Standard Time	Event Details Event Details	2/24/2021 Access Granted	34190668 232746517	60 60
OnGuard 7.4 Access Granted by Res OnGuard 7.4 Access Granted by Res	OF CRY, START DATE: 2/1/2021 12:00:00 AM: FND DATE: 3/2/2021 11:59:59 FI	M: READERS: DAX7-1.1:08-PICK UP RM RV, DAX7-1.2.08-BREARGOOM EN	Report Uste: 3/2/2021 1:57:42PM Eastern Standard Time	Eyent Details	2/24/2021 Access Granted	341912345 436187663	71 72
Dir Suand 7.4 Access Stented by Res	for CUJERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 P	M: READERS: DAX7-1,2:03-PICK UP RM IN, CAU7-1,3L04-BIGEAGRICUM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/24/2001 Access Grented	251303698	73
OnGuard 7.4 Access Strepted by Rea OnGuard 7.4 Access Granted by Rea		LA- READERS: DAX2-1.2.02-PICK UP RM IN, DAX2-1.2.04-BILEAKROKIM ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Detail:		341690520 341923609	1 2
On Buerd 7.4 Access Granted by Rev On Guard 7.4 Access Granted by Rev	CUIDNY STANT DATE: 7/1/2021 12:00:00 AM: FND DATE: 3/2/2021 11:59:59 P	M: READERS: DAX7-1.2.08-PICK UP RM IN, CAXT-1.2.04-BREAKHOOM EN1	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detail	s 2/25/2021 Access Branted	341920090	3
OnSuard 7.4 Access Granted by Res On Suard 7.4 Access Granted by Res	66 ONJERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 P 67 ONJERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 P	M; READERS: DAXY-1.2.03-PICK UP RIM IN, DAXY-1.2.04-BREAKROOM ENT M; READERS: DAXY-1.2.03-PICK UP RIM IN, DAXY-1.2.04-BREAKROOM ENT	Report Date: 3/7/2021 1:57:42PM Sestem Standard Time Report Date: 3/2/2023 1:57:42PM Sestem Standard Time	Event Detail	s 2/25/2021 Access Granted	341926798	5
OnGuard 7.4 Access Granted by Re-	der QUENY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 P	M: READERS: DAXX-1.2.03-PICK UP RM W, DAXX-1.2.04-IREARROUM EN	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detail	s 2/25/2021 Access Granted	341891027 341890233	7
OnGuard 7.4 Access Branted by Re OnGuard 7.4 Access Granted by Re	App. 10 (60): START DATE: 2/1/3001 12:00:00 AM: END DATE: 3/2/2021 11:59:59 P	M: READERS: DAXY-1.2.03-PICK UP RIM IN, DAXY-1.2.04-BREADKOOM KAT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detel	2/25/2021 Access Greened	436192047 341924226	5. 9
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On@used 7.4 Access Granted by Re	der QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 F	MA: READERS: DAX7-1.2.09-PICK UP RM 24, DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Detail Event Catal	ls 2/25/2021 Access Granted	341924296	12
OnGuard 7.4 Access Granted by Re OnGuard 7.4 Access Granted by Re	der QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59	PAA - READERS: DAX7-1.2 III PICK UP RIM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detail	lx 2/25/2091 Access Granted	341886220 341912991	13 14
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On Stand 7.4 Access Granted by Ri	CLIERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59	PM; READERS: DAX7-1,2,08-PKX UF RM IN, DAX7-1,2,04-BYBDXRS-URI BWI PM; READERS: DAX7-1,2,03-PKX UF RM IN, DAX7-1,2,04-BYBAKKOOM ENT	Report Date: 9/2/2021, 1:57:42PM Eastern Standard Time	Byant Detail	ls 2/25/2021 Access Granted		17
OnGuard 7.4 Access Granted by Ri	OLIGITY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/2/2021 11:59:59	PM; READERS: DAX7-1.1.03-PICK UP RM (N, DAX7-1.2.04-BREAKROOM ENT PM; READERS: DAX7-1.1.04-PICK UP RM (N, DAX7-1.2.01-BREAKROOM ENT	Report Date: 9/2/2021 1:57:42PM Eastern Standard Time Report Date: 2/2/2021 1:57:42PM Eastern Standard Time	Event Detail	lls 2/25/2021 Access Granted	341924402	19
OnGuard 7.4 Access Granted by Re OnGuard 7.4 Access Granted by Re	other GUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59	PM: READBIS: DAXY-3,2.08-PICK UP RM IN, DAXY-1.2.04-BREAKHOOM KNI	Report Date: 3/2/2021 1:57542PM Eastern Standard Time Report Date: 3/2/2021 1:57;42PM Eastern Standard Time	Event Date	lls 2/25/2021 Access Grented		20 21
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OnGuard 7.4 Access Granted by Ri OnGuard 7.4 Access Granted by Ri	eder CUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/2/2021 11:59:59	PM: READERS: DAXO-1.2.09-PICK UP RM IN, DAXO-1.2.04-BREAKROOM ENT PM: READERS: DAXO-1.2.03-PICK UP RM IN, DAXO-1.2.04-BREAKROOM ENT	Report Date: 3/1/2021 1:57:42PM Eastern Standard Time	Event Data	dis 2/25/2021 Accuse Grantee	341813079	24 25
OnGuard 7.4 Access Greated by R	ador QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2021 11:59:59	#M: READERS: CAX7-1.2.03-PICK UP RM IN, DAX7-1.7.04-BREAKROOM ENT	Report Oats: a/2/2021, 1:57:42PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Deta	lls 2/25/2021 Access Grantes	341925548	512
OnGuerd 7.4 Access Granted by R OnGuard 7.4 Access Granted by R	wdw	PM: READERS: DAX7-1.2.03-PICK UP RM IN, CACT-1.2.04-BREADERS ON ENTIRE	Report Date: 3/1/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Tires	Event Date	ilis 2/25/2021 Apoess Brantes		27 28
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OnGuard 7.4 Access Granted by R	HIGH CUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:58	PM: READERS: DAXY-1.2.03-PICK UP RM IN, DAXY-1.2.08-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 2/2/2021 1:57:43PM Eastern Standard Time	e Svent Deta	ulis 2/25/2021 Access Grante	341913365	31.
OnGused 7.4 Access Granted by F OnGused 7.4 Access Granted by F	moder QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2021 11:59:59	PM: READERS: DAX7-1.2:09-PICK UP HM IN, DAX7-1.2:04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Entern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	p Event Deta e Event De∜i	ells 2/25/2011 Access Grante	d 289318546	92. 39
On@uard 7.4 Access Granted by F On@uard 7.4 Access Branted by F	ender QUERY; START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:55	PM: READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	 Event Des 		d 341906868	34 35
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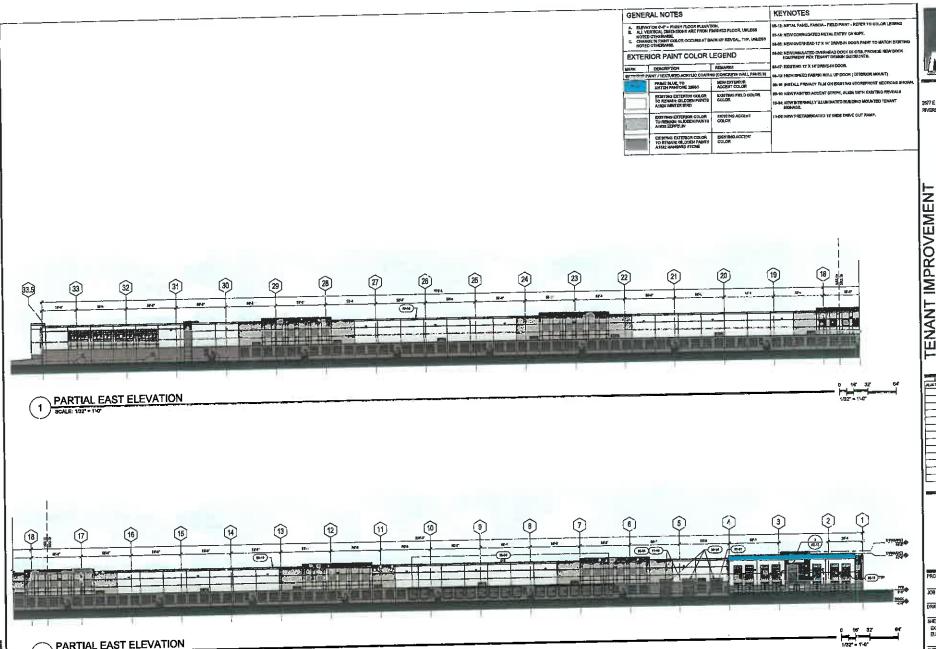
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OnBuerd 7.4 Acress Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; GND DATE: 3/2/2021 11:50	HSG PM; READERS: DAXY-1.2.03-PKX LIP RM IN, DAXY-1.2.04-BREAKROOM ENT		Event Details Event Details		341906668 282910862	42 43
OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Branted by Reader	DUEDY- START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/1/2021 11:59	#56 PM; READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.01-BREAKROOM ENT #56 PM; READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/2/202). 1:57:42PM Eastern Standard Time	Ivent Details	2/25/2021 Access Grented	341895807 341925872	44 45
On@uard 7.4 Access Granted by Reader	CLERY- START DATE: 2/1/2021 12:00:00 AM: RND DATE: 3/2/2021 11:50	9:59 PM; READERS: DAX7-1.2/3-PICK UP RM IN, DAX7-1.2/01-B/TEAKRODM ENT 9:59 PM; READERS: DAX7-1.2/35-PICK UP RM IN, DAX7-1.2/01-B/TEAKRODAN ENT		vent Details	2/25/2021 Access Grented	34 1891952	46
OnGuard 7.4 Access Granted by Reader Collumn 7.4 Access Granted by Reader	PRISHLY: START DATE: 1/1/2021 12:00:00 AM: END DATE: 1/2/2021 11:5	R:59 PM; READERS: DAX7-1.2,03-PICK UP RM IH, DAX7-1.2,04-BREAKROOM ENT				341922970 410432873	47 45
OnSuard 7.4 Access Stanted by Reeder OnGuard 7.4 Access Granted by Reeder	OLICBY: START DATE: 2/1/2021 12:00:00 AM: HND DATE: 3/2/2021 11:5	8-58 PM; READERS: DAX7-1,2,03-PICK UP RM IN, DAX7-1,2,04-BREAKROOM ENT 9-59 PM; READERS: DAX7-1,2,03-PICK UP RM IN, DAX7-1,2,04-BREAKROOM ENT	Report Date: 3/2/2071, 1:57:42PM Septem Standard Time	event Details	2/25/2021 Access Granted	251306655 341908734	49 50
OnGuerd 7.4 Access Granted by Reader	CHERY: START DATE: 2/1/2021 12:00:00 AM: BND OATE: 3/2/2021 11:5	9:59 PM; RÉADERS: DAICY-1.2.08-PICK UP RM IN, DAICY-1.2.04-BREAKNOOM ENT R:59 PM; READERS: DAICY-1.2.09-PICK UP RM IN, DAICY-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Prent Details	2/25/2021 Access Granted	436186698	51
Jindhard 7.4 Jacobs Granued by Render	CHERT STURY DATE, 2/1/2011 11.0000 AM. BYO DATE: 5/1/2011 11:5	8.29 Pini; RZADEKS: DAX7-1.1.73-FLA UP RM (H, DAX7-1.2.04-BREATROOM ENT	Reputat Cinter: 3/2/2021 5.57 979 in Eastern StoroGere Time		2/25/2021 Access Granted 2/25/2021 Access Granted	541881721 2317@722	52 53
OnGuard 7.4 Access Stranted by Reader OnGuard 7.4 Access Stranted by Reader	DUFAY: START DATE: 2/1/2021 12:00:00 AM: BND DATE: 3/2/2021 11:5	9.59 PM; READERS; DAX7-1.2.09-PICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT 9.58 PM; READERS; DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time		2/25/2021 Access Granted 2/25/2021 Access Granted	134691775 410429965	54 55
OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:5	959 PM; READERS: DAX7-1.2.03-PICK LIP RM IN, DAX7-1.2.04-BREAKROOM ENT 959 PM; READERS: DAX7-1.2.03-PICK LIP RM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detells	7/25/2021 Access Granted	251394739	58
On Superil 7.4 Access Greated by Render	DUESN: START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11:5	9:59 PM; READERS: DAX7-1.2.09-PICK UP RM:N, DAX7-1.2.04-985AKROOM ENT 9:59 PM; READERS: DAX7-1.2.09-PICK UP RM:N, DAX7-1.2.04-98EAARDOM ENT			2/25/2021 Access Branted 2/25/2021 Access Branted	241906470 341019864	57 58
OnGuard 7.4 Access Granted by Reader DeGuard 7.4 Access Granted by Reader	QUIERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 13:5	9:59 PM; HÉADERS: DAXO-1.2.03-PICK UP RM RI, DAXO-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PW Eastern Standard Time	Event Datelle Event Datelle	2/25/2021 Access Granted 2/25/2021 Access Granted	341906822 281672745	59 90
OriGuard 7.4 Access Granted by Reader OriGuard 7.4 Access Granted by Reader	CHESTS START DATE: 2/1/2023 12:00:00 AM: END DATE: 3/2/2021 115	9:59 PM; READERS: DAUT-1,2,03-PICK UP RM IN, DAUT-1,2,01-EREAKROOM ENT 9:59 PM; READERS: DAXT-1,2,03-PICK UP RM IN, DAXT-1,2,04-EREAKROOM ENT	Report Date: 3/2/2021 1:57, 12PM Eastern Standard Time	Event Details	2/25/2021 Access Granted	280588558 289218791	61
OnSuand 7.4 Access Granted by Reader	OUGDN, START DATE: 321/2021 12:00:00 AM: FMD DATE: 3/2/2021 11:5	955 PM; NEADERS; DAXT-1.2.02-PICK UP RWIN, DAXT-1.2.04-BREAUTGOM ENT 1959 PM; READERS; DAXT-1.2.08-PICK UP RWIN, DAXT-1.2.04-BREAUTGOM ENT	Report Date: 3/2/2021 3:57:42PM Eastern Standard Time	Event Details Event Details	2/25/2021 Access Grented 2/25/2021 Access Grented	251394981	ΘΘ
OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Granted by Reader	OLICIAN START DATE: 2/1/2021 12:0050 AM: FND DATE: 3/2/2021 12:5	B-BI PM: READERS: DAX7-1.2.09-PICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT		Event Datalls Event Datalls	2/25/2021, Access Granted 2/25/2021, Access Granted	410440077 218611830	64 85
OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Granted by Reader	CURRENT PATE: 2/1/2021 12:00:00 AM: BND DATE: 3/2/2021 11:0	19:59 PM; READERS; DAXY-1,2,09-PICK UP RM IN, DAXY-1,2,04-BREAKROOM ENT 19:59 PM; READERS; DAXY-1,2,03-PICK UP RM IN, DAXY-1,2,04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Stendard Time	Event Detells	2/25/2021 Access Granted 2/25/2021 Access Sminted	341906668 232746517	66 67
OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Granted by Reader	DUERY: START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11:5	1956 PM; BEADERS: OAXT-1.2.03-PKX UP RM IN, DAXT-1.2.04-BREAKROOM ENT HISB PM; READERS: DAXT-1.2.03-PKX UP RM IN, DAXT-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datalis Event Details	2/25/2021 Access Grented	251906941	68
OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:3	9:59 PM; READERS: DAX7-1.1.03-PICK UP RM IN, DAX7-1.2.01-BREAKROOM ENT	Report Date: 2/2/2021 1:57:42PM Castern Standard Time Report Date: 3/2/2021 1:57:42PM Fautum Standard Time	Event Details Event Details	2/25/2021 Access Granted 2/26/2021 Access Granted	251394735 341918387	1
On Guard 7.4 Access Granted by Reader On Guard 7.4 Access Granted by Reader	OF BURY: START DATE: 3/1/2021 12:00:00 AM: SND DATE: 3/2/2021 11:	IB:59 PM., READERS. DAX7-1.2:08-P; CK UP'SM 'N. DAX7-1.2:04 SNEAKROOM ERT IB:59 PM.; READERS: DAX7-1.2:08-P; CK UP RM IN, DAX7-1.2:04-BREAKROOM ENT	Report Date: 3/1/2021, 1:57:42PM Eastern Standard Time	Event Details Event Details	2/26/2021 Access Granted 2/26/2021 Access Strented	841921808 486177793	2 %
OnGuard 7.4 Access Strented by Reader OnGuard 7.4 Access Grented by Reader	THE DAY- START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 115	19:55 PM; READERS; DAXO-1.2.03-PLOK UP RM (M, DAXO-1.2.04-BREAKROOM ENT 19:56 PM; READERS; QAXO-1.2.03-PLOK UP RM IM, DAXO-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Oate: 3/2/2021 2:57:42PM Eastern Standard Time	Event Details	2/26/2021 Access Stranged	341912662	4
OnGuard 7.4 Access Granted by Reader	OURDY: START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11:	59:59 PM; READERS: DAX7-1.2.03-PICK UP RM IN, CAX7-1.2.04-BREAKRIOM ENT 59:59 PM; READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 2:57:42PM Eastern Standard Time Report Date: 3/2/2022 1:57:42PM Eastern Standard Time	Event Details Event Details	2/26/2021 Access Granted 2/26/2021 Access Granted	341920002 343890101	5 6
On Suand 7.4 Access Spanted by Reader On Search 7.4 Access Spanted by Reader	CHIEFY: START DATE: 2/1/2021 12:00:00 AM; SND DATE: 3/2/2021 11:	59:59 PM; READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.03-BREAKROOM ENT	Report Date: 3/2/2021 1/57:42PM Seatern Steedard Time Report Date: 3/2/2021 1/57:42PM Seatern Steedard Time	Event Details Event Details	2/26/2021 Access Granted 2/26/2021 Access Granted	841892414 341892062	8
Driffuerd 7.4 Access Granted by Reader Driffuerd 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; BND DATE: 1/2/2021 11: QUERY: START DATE: 2/1/2021 12:00:00 AM; BND DATE: 3/2/2021 11:	59:59 PM; READERS; DAXT-1.2:03-PICK UP RM IN, DAXT-1.2:04-BREAKROOM ENT 59:58 PM; READERS; DAXT-1.2:03-PICK UP RM IN, DAXT-1.2:04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Beent Details	2/26/2021 Access Stended	241913101 436174709	9 10
OnGuerd 7.4 Access Grented by Resider	DUERY: START DATE: 2/1/2021 12:00:00 AM: END OATE: 3/2/2021 11:	SHISH PM; READERS: CAXT-1.2.03-PKX UP RM IN, DAXT-1.2.04-BREAXHOOM ENT SHISH PM; READERS: DAXT-1.2.03-PKX UP RM IN, DAXT-1.2.04-BREAKROOM ENT	Report Date: 1/1/2021 1:57:42PM Eastern Standard Time Report Date: 1/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/26/2021 Access Granted 2/26/2021 Access Granted	341890454	11
OnGuerd 7.4 Access Granted by Reader OnSuand 7.4 Access Granted by Reader	QUERY; START DATE: 2/1/2021 12:00:00 AM; END DATE: a/2/2021 11:	SR:59 PM; READERS: DAX7-1,2.03-PICK UP RM IN, DAX7-1,2.04-BREAKROOM ENT	Report Date: 3/2/2021 157:42PM Eastern Standard Time Report Date: 3/2/2021 157:42PM Eastern Standard Time	Event Details Event Details	2/26/2021 Access Granted 2/26/2021 Access Granted	341890233	12 13
OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Granted by Reader	OURSY: START DATE: 3/1/2021 12:02:01 AM: BND DATE: 3/2/2021 11:	59:59 PM; HEADERS: DAX7-1.2,03-PICK UP RM IN, DAX7-1.2,04-BREAKROOM ENT 59:50 PM; READERS: DAX7-1.2,03-PICK UP RM IN, DAX7-1.2,04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Sestern Standard Time	Event Details Event Details	2/16/2021 Access Granted 2/26/2021 Access Granted	341920107 341913079	14 15
OriGuard 7.4 Access Granted by Reader OriGuard 7.4 Access Granted by Reader	CHIERRY: START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11:	59:58 PM; RIEADERS: DAX7-1.2.08-PICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT SNESS PM; READERS: DAX7-1.2.03-PKX UP RM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 2/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:912PM Eastern Standard Time	Event Details	2/26/2021 Access Grented	498124721	16
OnGuard 7.4 Access Branted by Reeder	THEREY: START DATE: 2/1/2001 12:00:00 AM: END DATE: 3/2/2021 11:	59:59 PM; READERS; CAXY-1,2,03-PKK UP RM IN, DAXY-1,2,04-BREAKROOM ENT 59:59 PM; READERS; DAXY-1,2,03-PKK UP RM IN, DAXY-1,2,04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/26/2021 Access Granted 2/26/2021 Access Granted	341924094 341892767	17 18
OnGuard 7.4 Access Granted by Render OnGuard 7.4 Access Granted by Render	OLIFRY: START DATE: 3/1/2021 12:00:00 AM: BND DATE: 3/2/2021 11	59:59 PM; READERS: DAX7-1.203-PICK UP RM IN, DAX7-1.201-BIEAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datails Event Datails	2/26/2021 Access Sminted 2/26/2021 Access Sminted	341924395	19 20
OrrGuard 7.4 Access Granted by Reader OrrGuard 7.4 Access Granted by Reader	CHIERY: CTART DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11.	5958 PM; READERS: DAXX-1.2.05-PIX. UP RM IM, DAXX-1.2.04-BREAKROOM ENT 5958 PM; READERS: DAXX-1.2,(E)-PIX. UP RM IM, DAXX-1.2.04-BREAKROOM ENT	Report Oate: 3/2/2021 3:57:42PM Eastern Standard Time	Event Details	2/26/2021 Access Grented	456174401 34 1888208	21 22
On Strand 7.4 Access Stranted by Reader On Guard 7.4 Access Stranted by Reader	OLIGRA: STREET DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11	5659 PM; READERS; DAXY-1,2,03-PICK UP RM IN, DAXY-1,2,04-BREAKROOM ENT 5559 PM; READERS; DAXY-1,2,03-PICK UP RM IN, DAXY-1,2,04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PnA Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/26/2021 Access Granted 2/26/2021 Access Granted	426199060	29
OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; BND DATE: 3/2/2021 11	59:59 PM; READERS: DAX7-1.2.03-PICK UP 9M IN, DAX7-1.2.04-BIEACROCIM ENT 59:59 PM; READERS: DAX7-1.2.03-PICK IP RM IN, DAX7-1.2.04-BIEAKROCIM ENT	Report Date: 3/2/2021 1:57:42PM Fastern Standard Time Report Date: 3/2/2021 1:57:42PM Fastern Standard Time	Event Datalis Event Datalis	2/26/2023 Access Granted 2/26/2021 Access Granted	341590520 341913431	24 25
OnGuerd 7.4 Access Granted by Reader OnGuerd 7.4 Access Granted by Reader	29 PSRY: START DATE: 2/1/2021 12:00:00 AM: END DATE: H/2/2021 11	59:58 PM; READERS: DAX7-1.2.08-PICK UP RM IN, DAX7-1.2.04-BREAX90 OM ENT	Report Date: 1/2/2021 3:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Octobs Event Details	2/25/2021 Access Grented 2/26/2021 Access Grented	341972904 410492879	26 27
OnSuard 7.4 Access Strented by Reeder OnGuard 7.4 Access Granted by Reader	DUEDY- START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11	S859 PM; READERS: DAXY-1.2,(B-P)CK LIP RM IN, DAXY-1.2,04-88EAKROOM SNT LS859 PM; READERS: (DAXY-1.2,03-PICK LIP RM IN, DAXY-1.2,04-BREAKROOM SNT	Report Date: 3/3/2021 1:57:42PM Eastern Standard Time	Event Details	2/26/2021 Access Grented	341910916	28
CinGuard 7.4 Access Greened by Reader	CLEEDY START DATE: 2/1/2021 12:00:00 AM: FMD DATE: 3/2/2021 13	59:39 PM; READERS: CAX7-1.2,02-PICK UP RM IN, DAX7-1.2,04-BREAKROOM ENT :59:59 PM; READERS: DAX7-1.2,03-PICK UP RM IN, DAX7-1.2,04-BREAKROOM ENT	Report Date: 3/2/2021 1.57:42PM Eastern Standard Time Report Date: 3/2/2021 1.57:42PM Eastern Standard Time	Event Detells Event Datalis	2/25/2021. Access Grented 2/26/2021. Access Grented	341892084	29 30
On Suard 7.4 Access Granted by Reader On Suard 7.4 Access Granted by Reads	PAISSES START DATE: 2/1/2021 12:00:00 AM: FAID DATE: 3/2/2021 11	S959 PN: READERS: DAXY-12.09-PICK UP RM IN, DAXY-12.04-BREATHUSHIENT	Report Date: 3/2/2021 1:57:42PM Easiern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/26/2021 Access Granted 2/26/2021 Access Granted	343925218 343925548	31 32
OnGuard 7.4 Access Granted by Reads OnGuard 7.4 Access Granted by Reads	CLIERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 13	S959 PM; READERS: DAXO-1.2,09-PICK UP RM IN, DAX7-1.2,04-BREAKROOM ENT .S959 PM; READERS: DAX7-1.2,03-PICK UP RM IN, DAX7-1.2,04-BREAKROOM ENT	Record Date: 3/2/2021 157:42PM Eastern Standard Time	Event Dotalis	2/26/2021 Access Granted 2/26/2021 Access Granted	341859220 341590385	38 34
OnGuard 7.4 Access Granted by Reads OnGuard 7.4 Access Granted by Reads	QUERY: START CATE: 2/1/2021 12:00:00 AM; END DATE: 2/2/2021 17	:59:59 PM; READERS; CAX7-1.2/03-PICK UP RM IN, (18X7-1.2.04-BREADUDOM ENT ;58:59 PM; READERS; DAX7-1.2/03-PICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 8/2/2021 1:57/42PM Sestem Standard Time Report Date: 3/2/2021 1:57/42PM Eastern Standard Time	Event Details	2/28/2021 Access Sminted	341892924	35
OnGward 7.4 Access Granted by Reads	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11	:59:59 PM; READERS: DAXT-1.2.03-PICK UP RM UN, DAXT-1.1.04-BREADROOM ENT :59:59 PM; READERS: DAXT-1.2.03-PICK UP RM IN, DAXT-1.2.04-BREAKROOM ENT	Report Date: 1/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Cetails	, 2/26/2021 Acous Granted 2/26/2021 Acous Granted	410430031	36 37
Optioned 7.4 Access Granted by Rende OnGuard 7.4 Access Granted by Rende	OLIERY: START DATE: 2/1/2021 12:00:00 AM; SHD DATE: 3/2/2021 13	:59:59 PM; READERS; DAXZ-1.2.03-PICK UF RM IN, DAXZ-1.2.04-BREADROOM ENT	Report Date: 3/1/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/26/2021 Access Grented 2/26/2021 Access Grented	251334735 341906668	3B 39
OnGuard 7.4 Access Granted by Reads OnGuard 7.4 Access Granted by Reads	OUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 12: OUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 12:	LS9:59 PM; READERS; (JAX7-1,2,09-PICK UP RM IN, DAX7-1,2,04-BREADROOM ENT LS9:59 PM; READERS; DAX7-1,2,03-PICK UP RM IN, DAX7-1,2,04-BREADROOM ENT	Report Date: 3/2/2021, 157:42PM Eastern Standard Time	Event Datails	2/28/2021 Access Granted	232763722	40
OnGuard 7.A Access Granted by Reside	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/1/2021 13	LS9:59 PM; READERS: DAID-1,2:08-PICK UP RM IN, DAXT-1,2:04-BREAKROOM ENT LS9:59 PM; READERS: DAXT-1,2:08-PICK UP RM IN, DAXT-1,2:04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/26/2021 Access Granted 2/26/2021 Access Granted	436188896 217897972	41 42
On Guard 7.4 Access Granted by Reads On Guard 7.4 Access Granted by Reads	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 5/2/2021 1:	LSGSS PM: READERS: DAG-1.2.08-PICK UP RM IN, DAK7-1.2.09-BREAKROOM ENT	Report Date: 3/2/2021. 1:57:42PM Eastern Standard Time Report Date: 3/2/2021. 1:57:42PM Eastern Standard Time	Event Details Event Details	1/26/2021 Access Granted 2/26/2021 Access Granted	289219546 341906668	44
OnGuard 7.4 Access Granted by Reeds OnGuard 7.4 Access Granted by Reads	OUERY: START DATE: 2/1/2021 12:00:00 AM; BND DATE: 3/2/2021 12:00:00 AM; BND DATE: 3/2/2021 12:00:00 AM; BND DATE: 3/2/2021 1	LSB:59 PM; READERS: DAX7-1.2.03-PICK UF RM IN, DAX7-1.2.04-BREAKROOM ENT LSB:59 PM; READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Festern Standard Time	Eyent Details	2/26/2021 Access Branted 2/26/2021 Access Granted	289222124 341891952	46 46
OnGuard 7.A Access Granted by Reads OnGuard 7.A Access Granted by Reads	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 1	LS958 PM; READERS: DAXT-11.03-PICK LIP INN IN, DAXT-12.04-BREAKROOM ENT LS958 PM; READERS: DAXT-12.03-PICK LIP RM IN, DAXT-12.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Tirro Report Date: 3/2/2021 1:57:42PM Eastern Standard Tirro	Event Details Event Datails	2/26/2021 Access Granted	410492879	47
On Busing 7.4 Access Brented by Read	OUERT: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 1	1.5858 PM; READERS: DAID-1.2.08-PICK UP RM IN, DAIX-1.2.04-BREAKROOM ENT 1.58259 PM; READERS: DAXY-1.2.02-PICK UP RM IN, DAIX-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Detells Event Detells	2/26/2023 Access Granted 2/26/2023 Access Granted	45.6188698 251203698	48
OnGuard 7.4 Access Granted by Reed OnGuard 7.4 Access Granted by Reed	 QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 1 	1:59:59 PM; READERS: DAX7-1.2,03-PICE UP RM IN, DAX7-1.2.04-IREAKROOM ENT	Report Date: 3/2/2021 1:57:42FM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/25/2021 Access Branted 2/25/2021 Access Branted	241906734 410432873	50 51
OnGuard 7.4 Access Granted by Read- DeSumed 7.4 Access Granted by Read	 QUERY: START DATE: 2/1/2021 12:00:00 AAR; END DATE: 3/2/2021 1 QUERY: START DATE: 7/1/2021 12:00:00 AM; END DATE: 3/2/2021 3 	159:59 PM; READERS: DAIT-1.2.03-PICK UP RM IN, DAIT-1.2.04-BREADROOM ENT 1.59:59 PM; READERS: DAIT-1.2.03-PICK UP RM IN, DAIT-1.2.04-BREADROOM ENT	Report Date: 3/2/2011 1:57:42PM Sector Standard Time	Event Detalls	2/26/2021 Acress Grented 2/26/2021 Acress Grented	212763722 251334739	52. 38
On Guard 7.4 Access Granted by Head	 OUFRY: START DATE: 3/1/2021 12:00:00 AAA: END DATE: 3/2/2021 1 	1.59:59 PM; READERS: DAUT-1.2.09-PICK UP RRAIN, DAUT-1.2.04-BREAKEDOM ENT 1.59:59 PM; READERS: DAXT-1.2.03-PICK UP RRAIN, DAUT-1.2.04-BREAKEDOM ENT	Report Date: 3/2/2021, 1:57:42PM Sestem Standard Time Report Date: 3/2/2021 1:57:42PM Seatern Standard Time	Event Details Eyent Details	2/26/2021 Access Sminted	251303688	54
On Suand 7,4 Access Granted by Read On Suand 7.4 Access Branted by Read	 CHERY: START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 1 	1:59:59 PM; READERS: DAX7-1.2.03-PICK UP RIM IN, DAX7-1.2.04-BREADROOM ENT	Report Date: 3/2/2021, 1:57:42FM Eastern Standard Time Report Date: 3/2/2021, 1:57:42FM Eastern Standard Time	Event Details Event Details	2/26/2021 Access Granted 1/26/2021 Access Granted	436192047 241508144	95 56
OnGuard 7.4 Access Granted by Reed OnGuard 7.4 Access Granted by Reed	 OLICHY: START DAYS: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 5 	15959 PM; READERS; DAXF-1,2,03-PICK UP RANIN, DAXF-1,2,04-BREAXROOM ENT 1,5959 PM; READERS; DAXF-1,2,05-PICK UP RANIN, DAXF-1,2,04-BREAXROOM ENT	Report Date: 3/2/2021 1:57:42PM Sentern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Datails	2/26/2021 Access Granted 2/26/2021 Access Granted	341906470 341919884	57 58
On Suand 7.4 Access Grented by Read On Suand 7.4 Access Grented by Read	 DUEBY: START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 3 	1:59:59 PM; READERS: DA07-1.2.09-PICK UP RM IN, DAX7-1.2.04-BEAGROOM ENT 1:59:59 PM; READERS: DAX7-1.2.09-PICK UP RM IN, DAX7-1.2.04-BEAGROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Eyent Details	2/28/2021 Access Granted	289216731	59
OnGuera 7.4 Acress Grented by Read	IT QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2021 1	159:59 PM; READERS: DAX7-12.03-PICK UP RM IN, DAX7-12.04-BREAXROOM ENT 159:59 PM; READERS: DAX7-12.03-PICK UP RM IN, DAX7-12.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Ostalis Event Details	2/26/2021 Access Granted 2/26/2021 Access Granted	410440077 841925929	60 61
Optioned 7.4 Access Granted by Resc Optioned 7.4 Access Granted by Resc	of Chery: Start Date: 2/1/2021 12:00:00 AM; END DATE: 3/2/2011	13-59-59 PM; READERS; DAXZ-1,2,03-PICK UP RM IN, DAXZ-1,2,04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/26/2021 Access Granted 2/26/2021 Access Granted	42-6190703 341906686	472 53
OnGuard 7.4 Access Granted by Reec OnGuard 7.4 Access Granted by Reec	** CHEERY: START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/1/2021 1	LISSES PM; READERS: DAUT-1.2.08-PICK UP RM IN, DAUT-1.2.01-BREAKROOM ENT LISSESS PM; READERS: DAUT-1.2.03-PICK UP RM IN, DAUT-1.2.01-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Titree	Event Ortalis	2/26/2021 Access Branted 2/26/2021 Access Granted	251308941 341891027	64 65
On Stand 7.4 Access Standard by Read On Stand 7.4 Access Standard by Read	W OUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 W OVERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021	11:59:59 PM; READERS: DAXY-12:03-PICK UP SM IN, DAXY-12:04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/26/2021 Access Granted	341926738	66
OnGuard 7.4 Access Stranged by Read	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021	11-59-59 PM: READERS: DAX7-1,2.08-PICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/25/2021, Access Granted 2/25/2021, Access Granted	232746517 410429343	67 68
DroGuard 7.4 Access Granted by Resi On Suard 7.4 Access Granted by Resi	W DUFRY: START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021	11:59:59 PM; READERS: DAX7-1,239-PICK UP RM IN, DAX7-1,2,04-BREAKROOM ENT 11:59:59 PM; READERS: DAX7-1,2,09-PICK UP RM IN, DAX7-1,2,04-BREAKROOM ENT	Report Date: 3/2/2021 3:57:42PM Eastern Standard Time	Event Details Event Details	2/28/2021 Access Granted 2/26/2021 Access Granted	341892084 251309032	70
OnGuard 7.4 Access Granted by Ree OnGuard 7.4 Access Granted by Ree	HE QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2021	11:59:59 PM; READERS; DAX7-12:03-PICK UP RM IN, DAX7-12:04-BREAKRIOM ENT 11:59:59 PM; READERS; DAX7-12:03-PICK UP RM IN, DAX7-12:04-BREAKROOM ENT	Report Date: 9/2/2021 1:57:62PM Sestem Standard Time Report Date: 8/2/2021 1:57:42PM Sestem Standard Time	Event Details	1/27/2021 Access Grented	410441531	1
On Guard 7.4 Access Granted by Rea	- OF SERVING ATTACK TO ATTE: 3/1/2011 12:00:00 AM: END DATE: 3/2/2021	11:58:89 PM; READERS: DAX7-12:03-PICK UP RM IN, DAX7-12:04-BREAKROOM ENT 11:59:89 PM; READERS: DAX7-12:03-PICK UP RM IN, DAX7-12:04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/27/2021 Access Strented 2/27/2021 Access Grented	436177753 341513367	3
On Guerd 7.4 Access Grented by Rea On Guerd 7.4 Access Grented by Rea	W QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021	11:59:59 PM: READERS: DAXY-1.2.03-PICE UP RM IN, DAXY-1.2.01-BREAKROOM ENT	Report Date: 3/2/2021 1:57:52PM Eastern Standard Time Report Date: 3/2/2021 1:57:52PM Eastern Standard Time	Syant Details Event Details	2/27/2021 Access Granted 2/27/2021 Access Granted	341892062 341919556	4 5
OnGuard 7.4 Access Granted by Rea On Guard 7.4 Access Granted by Rea	- OURSEY, START DATE: 2/1/2021 12:0000 AM: END DATE: 3/2/2021	11:59:59 PM; READERS: DAGY-1.2:03-PICK UP RM RI, DAGY-1.2:04-BREATROOM ENT 11:59:59 PM; READERS: DAGY-1.2:03-PICK UP RM IN, DAGY-1.2:04-BREATROOM ENT	Report Date: 9/2/2021 157:42PM Eastern Standard Time	Event Details	2/27/2021 Access Granted	426174709	6 7
On Guard 7.4 Access Granted by Ret	Ser QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021	1159:59 PM; READERS: DAXT-1.2:09-PICK UP RIM IN, DAXT-1.2:04-BREADROCHA ENT 11:59:59 PM; READERS: DAXT-1.2:09-PICK UP RIM IN, DAXT-1.2:04-BREADROCHA ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/27/2021 Access Strented 2/27/2021 Access Strented	341921603	8
OnGuard 7.4 Access Granted by Rea OnGuard 7.4 Access Granted by Rea	CHIERY: START DATE: 2/1/2021 12:00:01 AM: FND DATE: 3/2/2021	11.59:59 PM: READERS: DAX7-1.2/3-PICK UP RM IN, DAX7-1.2/3-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datails	2/27/2021 Access Granted 2/27/2021 Access Granted	341925372 341913101	9 10
On Bushed 7.4 Access Granted by Ret Griduard 7.4 Access Sterrited by Res	CUERY: START DATE: 2/1/2021 12:00:00 AM: END CATE: 3/2/2021	11:59:59 PM; READERS: DAIG-1.2:09-PICK UP RM IN, DAIG-1.2:09-BREAKROOM ENT 11:59:59 PM; READERS: DAIG-1.2:03-PICK UP RM IN, DAIG-1.2:04-BREAKROOM ENT	Report Date: 3/2/2021, 3:57:42PM Eastern Standard Time	Event Details Event Details	2/27/2021 Access Grented 2/22/2021 Access Grented	341912881 341913078	11 12
OnGuero 7.4 Access Strented by Rev Designed 7.4 Access Granted by Rev	der GUERY: START DATE: 2/1/802) 12:00:00 AM; END DATE: 3/2/2021	115959 PM; READERS: CAXT-1.2.03-PICX UP RM IN, DAXT-1.2.04-BREARROOM ENT 115959 PM; READERS: DAXT-1.2.03-PICX UP RM IN, DAXT-1.2.04-BREARROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/27/2021 Access Greated	218611830	13
OnGuard 7.4 Access Streeted by Res	Aug DIJERY: START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021	11:59:59 PM; READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT 11:59:59 PM; READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 9/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/27/2021 Access Grented 2/27/2021 Access Stanted	341913139 436174401	14 15
On Guerd 7.4 Access Granted by Re- On Guard 7.4 Access Granted by Re-	der CHIENY: START DATE: 2/1/202) 12:00:00 AM; END DATE: 3/2/2021	11:59:59 PM; READERS: DAXT-1.2.03-PICK UP RM IN, DAXT-1.2.04-BIEAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/22/2021 Access Granted 2/27/2021 Access Granted	343893066	16 17
On Guard 7.4 Access Granted by Re- On Guard 7.4 Access Branted by Re-	Aur OLIERY: START DATE: 3/1/2021 12:00:00 AM: END DATE: 3/2/2021	11:56:58 PM; READERS: CAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREAGROOM ENT 11:59:59 PM; READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREAGROOM ENT	Report Date: 8/2/2021 1:57:42PM Eastern Standard Time	Event Detells	2/27/2022 Access Granted	341920107 4x6192950	18 19
OpGuard 7.4 Access Granted by Re	der QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021	11.59:59 PM; READERS: DAKY-1.2.03-PICK UP RM IN, DAKY-1.2.04-BREAKROOM ENT 11.59:59 PM; READERS: DAKY-1.2.03-PICK UP RIM IN, DAKY-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/27/2021 Access Stanted 2/27/2021 Access Stanted	341892569	20
On Guard 7.4 Access Granted by Re On Guard 7.4 Access Granted by Re	A DELIBERY STANS THATE: 2/1/2021 12:00:01 AM: FND DATE: 3/2/2021	. 11-59:55 PM: HEAZIERS: DIAXY-1,2,09-PICK UP RM (N. DAXY-1,2,04-BREAXRODM ENT	Report Date: 1/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:43PM Eastern Standard Time	Event Details Event Details	2/27/2021 Access Greated 2/27/2021 Access Greated	43 61 93 080 34 19 10986	21 22
On Suard 7.4 Access Granted by Re On Suard 7.4 Access Granted by Re	Apr. CHERY: START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021	11:58:59 PM; READERS: DAX7-12.03-PICK UP RM IN, DAX7-12.04-BREAKROOM ENT 11:58:59 PM; READERS: DAX7-12.03-PICK UP RM IN, DAX7-12.04-BREAKROOM ENT	Report Date: 3/2/2021, 1:57562PM Eastern Standard Time	Event Details Event Details	2/27/2021 Access Granted	941918622 841891532	25 24
On Guard 7.4 Access Granted by Re On Guard 7.4 Access Granted by Re	outery: START DATE: 2/1/2021 12:00:00 AM; END DATE: 9/2/2021	L1159:59 PM; READERS; DAXX-1,2,09-PICK UP RIM IN; DAXX-1,2,04-BREAKROOM ENT L1159:59 PM; READERS; DAXX-1,2,09-PICK UP RIM IN; DAXX-1,2,04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/27/2021 Access Granted	241922904	25
OnGuerd 7.4 Access Granted by Re	der QUERY: START DATE: 2/1/2021 12:00:00 AM; BND DATE: 3/2/203	11:59:59 PM; READERS: DAX7-1.2,02-PICK UP RM IN, DAX7-1.2,04-BREAKROOM ENT 11:59:59 PM; READERS: DAX7-1.2,03-PICK UP RM IN, DAX7-1.2,04-BREAKROOM ENT	Report Date: 9/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:43PM Eastern Standard Time	Event Details Event Details	2/27/2021 Access Granted	341913365 438178476	26 27
On Stand 7.4 Access Granted by Re Godused 7.4 Access Granted by Re	der QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/202	L 11:59:59 PM: READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/1/1021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time			341892414 341925548	28 29
OnGuard 7.4 Access Granted by Re OnGuard 7.4 Access Granted by Re	-4- OLERY: START DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/202	3 11:59:59 PM; READERS: DAX7-1.2.09-PICK UP RIAHM, DAX7-1.2.04-9REAKROOM ENT 3 11:59:59 PM; READERS: DAX7-1.2.02-PICK UP RIAHM, DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:02PM Eastern Standard Time	Event Detail:	2/27/2021 Access Granted	341892084 436174665	3D 31
On Guard 7.4 Access Granted by Re ConGuard 2.4 Access Granted by Re	OUENY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/202	1 11:59:59 PM; READERS; DAX7-1,2.03-PICK UP RM IH, DAX7-1,2.04-BREAKADOM ENT 1 11:59:59 PM; READERS; DAX7-1,2.03-PICK UP RM IN, DAXC-1,2.04-BREAKROOM ENT	Report Date: 9/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/27/2021 Access Sminted	341912682	32
On Sound 7.4 Across Granted by It		1 11:93:99 PM; READERS; DAXT-1.2.03-PICK UP RM IN, DAXT-1.2.04-BREAKROOM ENT 1 11:59:59 PM; READERS; DAXT-1.2.03-PICK UP RM IN, DAXT-1.2.04-BREAKROOM ENT	Report Date: 3/2/1021 2:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/27/2021 Access Granted	410432873 341852976	35. 34
On Buard 7.4 Access Granted by R Un Guard 7.4 Access Granted by R	AND DATE: 3/2/202	1 11:59:59 PM: READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2:04-BREAKROOM ERT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Date	2/27/2021, Access Greented	341922970 341925218	35 36
OnGuard 7.4 Access Granted by R On Buard 7.4 Access Granted by R	ader OUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/202	1 11:59:59 PM; READERS: DAX7-12.03-PICK UP RM M, DAX7-12.04-BREAKROOM ENT 1 11:59:55 PM; READERS: DAX7-12.08-PICK UP RM IN, DIXX7-12.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Debil	2/27/2021 Access Granted	426174379 241925950	97 98
OnGuerd 7.4 Access Granted by R OnGuerd 7.4 Access Granted by R	wider OUERY: START DATE: 2/1/2021 12:00:00 AM; BND DATE: 2/2/202	111:59:59 PM; READERS: DAX7-12,09-PICK UP RM (N, DAX7-12,04-BREAKROOM ENT 111:59:59 PM; READERS: DAX7-12,03-PICK UP RM (N, DAX7-12,04-BREAKROOM ENT	Report Date: 9/2/2021 1:57:42PM Eastern Standard Time Report Data: 3/2/2021 1:57:42PM Eastern Standard Time	Event Date!	s 2/27/2021 Access Granted	341891731	29
On Suand 7.4 Access Granted by R	OUERY: START DATE: 2/1/2011 12:00:01 AM; END DATE: 3/2/201	II 1159:99 PM; READERS: DAXT-1.2.03-PICK UP RM IN, DAXT-1.2.04-89EAKROOM ENT II 1159:59 PM; READERS: DAXT-1.2.08-PICK UP RM IN, DAXT-1.2.04-9/JEAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Featern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detail	5 7/27/2021 Access Grented		40 41
On Guard 7.4 Access Stanted by R On Guard 7.4 Access Branted by R		13 11:5959 PM; READERS: DAOC+1.2.05-PKX UP RM IN, DAX7-1.2.04-BREAKNOOM ENT	Report Date: 3/2/2021 3:57:42PM Eastern Stendard Time	Evant Datal			42

			2/27/2023 Access Granted	341906668	43
DISCURD 7.4 Access Granted by Render QUERY: START DATE: 2//2021 12:00:00 AMT; END DATE: 3/2/2021 11:30:00 PM; READERS: DATY-1_2:00-PVX; UP RM MI, DATY-1_2:00-BREAUCOOM ENT ORGANIZATION OF THE PROPERTY OF TH	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/23/2021 Access Granted	941891665	44
On Grant I 7.4 Access Granted by Reader DUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2.03-PKX UP RM IN, DAX7-1.2.03-BREAKROOM ENT		Event Details	2/27/2021 Access Granted 2/27/2021 Access Granted	217837972 261659595	45 46
OnGuard 7.4 Access Smarked by Rander QUERY: START PATE: 1/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:59:59 PM; REDDERS: DAX7-12.06-PICK UP RIM IN, DAX7-12.04-PICK UP RIM IN, DAX7-12.04	Report Date: 2/2/2021, 1:57:43PM Eastern Standard Time	Event Details	2/27/2021, Access Granted 2/27/2021, Access Granted	341922970 410482873	47 45
On Guard 7.4 Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 DATE: 3/2/2021 11:59:59 PM; READERS: DAXT-12:00-910X 19: DAXT-12: DAXT-1	Report Date: 3/2/2021 1:57:43PM Eastern Standard Time	Event Details	2/27/2021 Access Granted	341891665	49
On Swand 7.A Access Granted by Readers QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1-2-10-PICK UP RM IN, DAX7-1-2-04-READERSCOM ENT	Report Data: 2/2/2021 1:57:52PM Eastern Standard Time Report Data: 3/2/2021 1:57:52PM Eastern Standard Time	Event Datells Event Datells	2/27/2021 Access Granted 2/27/2021 Access Granted	436192950 341692084	50 51
O/GRAND 7.4 Access Granded by Reader O/ERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 12:00:00 PM; READERS: DAX7-12-03-PM; READERS: DAX7-12-03-PM; MID, DAX7-12-03-PREADERS DAX7-12-03-PREADERS: DAX7-12-03-PREADERS D	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Dutels	2/27/2021 Access Granted	341891781	52 53
OnGuard 7.4 Access Granted by Reader QUERY: START DATE: 2/1/2021 12:0000 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAUG-L1.09-PICK UP RIAMIN, DAXY-L2.04-READROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/27/2021 Access Granted 2/27/2021 Access Branted	436182950	54
OnGuard 7.4 Access Granted by Header QUERY; START DATE: 2/1/2021 12:00:00 AM; END DATE: 8/2/2021 11:59:59 PM; READERS: DAX7-12:08-97CK UP RM IN, DAX7-12:09-88FBAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/27/2021 Access Granted 2/27/2021 Access Granted	410439965	55 56
OFFIGURE 2. A RESEAS SERVING BY Reader CUESTY: START DATE: 2/1/2021 12:00:00 ANE, END DATE: 1/2/2021 11:00:00 ANE, END DATE: 1/2/202	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datells	2/27/2021 Access Granted	251334739	57
On Guard 7.4 Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-L2.03-FICK UP RM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 8/2/2021 1:57:42PM Seatem Standard Time Report Date: 8/2/2021 1:57:42PM Seatem Standard Time	Event Details Event Details	2/27/2021 Access Granted 2/27/2021 Access Granted	341906470 251307818	58 59
CHARLES OF THE PROPERTY OF THE	Report Date: 3/2/2021, 1:57:42FM Eastern Standard Time	Event Details	2/27/2021 Access Granted 2/27/2021 Access Granted	341908144 410440077	න වෙ
ONDWARD 7.4 Access Greented by Render QUERY: START DATE: 27/2021 12:00:001 ANC; RND DATE: 37/2021 11:59559 PM; READERS: DADY-1.2.03-PDCK IP RM IN, DATY-1.2.04-BEARDOON FMT OF START OF	Report Oate: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/27/2021. Access Granted	434077466	62
DisGuerd 7.4 Access Strented by Reader QUERY; START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/2/2021 11:50:59 PM; READERS: DAX7-1.2,03-9/CR UP RM IN, DAX7-1.2,04-9/EARDOM ENT	Report Date: 1/2/2021 1:57-42PM Eastern Standard Time Report Date: 1/2/2021 1:57:12PM Eastern Standard Time	Event Ontalls Event Ontalls	2/17/2021 Access Granted 2/17/2021 Access Granted	341925923 341908668	63 64
OFFICE 2/1/2021 12:50:50 PM; READERS: DAX7-12:03-PICK UP RM IN, DAX7-12:04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Tiree	Beent Datells	2/27/2021 Access Granted 2/27/2021 Access Granted	341891027 251908941	65 69
Opposed 74 Acres Generally Resident Cluent: START DATE: 2/1/2011 12:0000 AM; BND DATE: 3/2/2011 11:5959 PAI; READERS: DADG-12:05-PICK IP RM NR, DADG-12:06-PICK IP RM NR, DADG	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 1/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/27/2021 Access Granted	343926738	67
On Superd 7.4 Access Spinned by Reader OVERY: START DATE: 2/1/2021 32:00:00 AM; END DATE: 3/2/2021 11:58:59 PM; READERS: DAX7-1.2:08-PICK ID RM IN, DAX7-1.2:04-REAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/27/2021 Access Grented 2/28/2021 Access Grented	436187660	1
OnStand 7.4 Access Granted by Resider OLIGHT: START DATE: 21/2/2011 12:00:00 AM; END DATE: 31/2/2011 13:00:00 PM; READERS: OAXT-12:00-PICK UP RM IN, DAXT-12:00-PICK UP RM IN,	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Dutalls	2/28/2021 Access Granted	436177793	2
OnSwird 7.4 Access Granted by Resider QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:59:59 PM; READERS: DAX7-12:03-FICK UP RM IN, DAX7-12:04-BREAKROOM ENT	Report Date: 3/1/2021 1:57:42PM Eastern Standard Time Report Date: 1/1/2021 1:57:43PM Eastern Sucrelard Time	Event Details	2/25/2021 Access Granted 2/22/2021 Access Granted	4264527 34264528	3
December 1.4. Assess Company by Reader CUERC: START DATE: 2/1/202112:00:00 AM: END DATE: 3/2/202111:59:59 PM; READERS: DAX7-1.2:09-PXX UP RM IN, DAX7-1.2:04-BXSAXROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Univ. Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/28/2021 Access Granted 2/28/2021 Access Granted	341912991 436192047	5 6
ORGANITY A Access Granted by Reader CUESY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:00:00 PM; IEMDERS: DADT-12-08-PCX UP RM RI, DATE-12-09-READERS DADT-12-08-PCX UP RM RI, DATE-12-09-READERS DADT-12-09-PCX UP RM RI, DATE-12-09-READERS DADT-12-09-PCX UP RM RI, DATE-12-09-READERS DADT-12-09-PCX UP RM RI, DATE-12-09-PCX UP RM RID	Report Date: 3/2/2021, 1:57:A2PM Eastern Standard Time	Event Datells	2/28/2023 Access Granted	3419G8144	7
On General 7.4 Access Graphed by Reader QUESTY: START DATE: 2/1/1021 12:00:00 AM; END DATE: 3/2/1021 12:59:59 PM; READERS: DAXT-1.2:00-FICE UP RIM IN, DAX7-1.2:04-REAKROOM ENT	Report Date: 3/2/2021, 1/57:429M Eastern Standard Time Report Date: 8/2/2021, 1/57:429M Eastern Standard Time	Beent Datalis Event Octalis	2/28/2021, Access Granted 2/28/2021, Access Granted	341913185 486174401	9
Opfigued T.A. Access Granted by Reader Opfigu	Report Date: 3/2/2021 1:57:42PM Existent Standard Time	Event Details Event Details	2/28/2021 Access Granted 2/28/2021 Access Granted	341925548	10 11
OnSum 7.4 Access General by Reader CLERY: START DATE: 2/1/X021 12:00:00 AM; END DATE: 1/1/X021 11:95:59 PM; READERS: DAXY-1.2,05-PXX UP 1M-IN, DAXY-1.2,104-8856X800M ENT ORGANI 7.4 Access General by Reader QUERY: START DATE: 2/1/X021 12:00:00 AM; END DATE: 1/1/X021 11:95:59 PM; READERS: DAXY-1.2,05-PXX UP 7M IN, DAXY-1.2,06-PXX	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Apport Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datelle	2/28/2021 Access Granted	341891098	12
On Sunrid 7.4 Access Grapted by Resider QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:59:59 PM; READ BRS: DAX7-1.2:08-PICK UP RM IN, DAX7-1.2:08-BRS: DAX7-1.2:08-PICK UP RM IN, DAX7-1.2:08-BRS: DAX7-1.2:08-PICK UP RM IN, DAX7-1.2:08-BRS: DAX7-1.2:08-PICK UP RM IN, DAX7-1.2:08-PICK UP RM IN DAX7-1	Report Date: 19/2/2021. 1,57:42PM Section Steedard Time Report Date: 19/2/2021. 1,57:42PM Septem Standard Time	Event Details Event Details	2/28/2021 Access Granted 2/28/2021 Access Granted	341919644 341920107	13 24
ORGANI 7.A Across Grandel by Bassler OUERT: STATI DATE: 2/1/2021 12:0000 AM; END DATE: 3/1/2021 11:50:50 PM; READERS: OAXT-1.2.03-7/CX UP RM IN, DAXT-1.2.04-8EEARRICOM ENT ORGANI 7.A Across Grandel by Rassler OUERT: STATI DATE: 2/1/2021 12:0000 AM; BIO DATE: 3/1/2021 11:50:59 PM; READERS: DAXT-1.2.03-7/CX UP RM IN, DAXT-1.2.04-8EEARRICOM ENT	Report Date: 2/2/2021 1:57:42FM Eastern Standard Time	Event Details	2/28/2021 Access Granted	41,0440209 343926527	15 16
Oxigured 7.4 Access Granted by Reside: QUERY: START DATE: 2/1/201112:00:00 AM; END DATE: 3/2/201111:59:59 PM; READERS: DAX7-1.2-1-FICKUP RMIN; DAX7-1.2-0-BREAGROUM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/28/2021 Access Granted 2/28/2021 Access Granted	436174731	17
On Grant 17.4 Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXY-1.2.03-PICK UP RAN IN, DAXY-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 157:42PM Eastern Standard Time Report Date: 3/2/2021 157:42PM Eastern Standard Time	Event Details Event Details	2/28/2021 Access Granted 2/28/2021 Access Granted	436178476 438174665	18 19
Operand T.A. Access Sentend by Reader: QUERY: START DATE: 2/1/2021 12:0000 AM; SHD DATE: 3/1/2021 13:50:50 PM; READERS: DAXT-1.208-PDX UP RM IN, DAXT-1.204-READERS DAXT-1.208-PDX UP RM IN, DAXT-1.208-PDX UP	Report Date: 3/2/2021, 1:57:42FM Eastern Standard Time	Beent Details	2/26/2021 Access Branted	341891533	20
OriGuard 7.4 Access Granted by Render QUERY: START DATE: 2/1/2021 12:00:00 AM; BND DATE: 2/2/2021 11:50:59 PM; READERS: QAX7-12:00-PICK UP RMIIN, DAX7-12:04-BREARROOM ENT	Report Date: 1/1/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/28/2021 Access Granted 2/28/2021 Access Granted	410497879 341892084	21 22
On Guard 2.4 Access Granted by Render QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXY-1.2/03-PKX UP RM IN, DAXY-1.2/04-BREAKROOM ENT	Report Date: 3/2/2021 157:42PM Eastern Standard Time Report Date: 3/2/2021 157:42PM Eastern Standard Time	Event Details Event Details	2/28/2021. Access Granted 2/28/2021. Access Granted	34)892414 34186900	23 26
Oyelluard 7.A. Access Graphed by Nasider QUEXT: START DATE: 2/1/2021 12:0000 AM; END DATE: 3/1/2021 11:59:59 PM; READERS: DADD-1_CUE-PM; IN, DAXT-1_DA-BREAUSOOM ENT Operated 7.A. Access Graphed by Naside QUEXT: START DATE: 2/1/2021 12:0000 AM; END DATE: 3/1/2021 11:58:59 PM; READERS: DAXT-1_COE-PM2 IN DAXT-1_DA-BREAUSOOM ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details	2/25/2021 Access Branted	341892569	25
On Guard 7.4 Acoust Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1-2:09-PICK UP RM IN, DAX7-1-2:04-BREAGROOM ENT	Report Date: 3/1/2021 1:57:42PM Eastern Standard Time Report Date: 5/2/2021, 1:57:42PM Eastern Standard Time	Event Details Event Details	2/28/2021 Access Granted 2/28/2021 Access Granted	341892326 341919101	26 27
THERE STATES AND A SECOND REPORT OF THE STATES AND ASSESSED AND ASSESSED AND ASSESSED AND ASSESSED AND ASSESSED	Report Date: 3/2/2021 157/42PM Eastern Standard Time	Event Details Event Details	2/28/2021 Azones Granted 2/28/2021 Azones Granted	341925350 3418917#1	28 29
Optioned 7.4 Access Seminated by Brader QUEST: \$1387 DATE: \$2/1/2021 12:05:00 AM; END OATE: \$2/1/2021 11:05:09 PM; RELDERIS: DACT-1.2.07-HXX UP NM (N, DATT-1.2.07-88EAKROOM ENT Optioned 7.4 Access Seminated by Brader Optioned 7.4 Access Seminated by Brader QUEST: \$1387 DATE: \$2/1/2021 12:00:00 AM; END DATE: \$2/1/2021 12:00:00	Report Date: 3/2/2021. 1:57:42PM Eastern Standard Time Report Date: 3/2/2021. 1:57:42PM Eastern Standard Time	Event Ontalis	2/28/2021 Actes Granted	436197950	30
On Sugard 7.4 Access Strantad by Reader QUERY: START DATE: 2/1/2021 12:00:00 AN; END DATE: 3/1/2021 11:50:50 PM; EMDERS: DATG-1,2:0-FICEUP RM M; DAYG-1,2:0-BEARDOOM ENT	Report Date: 3/2/2021 1:57:42FM Eastern Standard Time Report Date: 3/2/2021 1:57:42FM Eastern Standard Time	Event Details	2/25/2021 Access Granted 2/25/2021 Access Granted	341906668 410493578	31 32
CONTROL TO A SHARE CONTROL TO BEEN START DATE: 2/1/2021 12:00:00 AM: END DATE: 2/2/2021 11:59:59 PM; READERS; DAV7-1.2:00-PICK UP RIM IN, DAV7-1.2:04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Restern Standard Time	Event Datells	2/28/2021 Access Granted 2/28/2021 Access Granted	235838848 341906666	33 34
Oviduard 7.4 Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DATF-12.03-FICE UP RM (N, DATF-12.03-BREAGROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Feature Standard Time	Event Details Event Details	2/2E/2021 Access Strented	241291665	36
On Guard 7.4 Access Hannad by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 12:00:29 PM; READERS: DATA-1.2/E-PICK UP RAI RI, DATA-1.2/E-PICK UP RAI RI RID RID RID RID RID RID RID RID RID	Report Date: 3/3/2021 1:57:42FM Eastern Standard Time Report Date: 3/2/2021 1:57:42FM Eastern Standard Time	Everyt Details Everyt Details	2/26/2021 Access Brented 2/26/2021 Access Granted	289219546 261889595	96 27
On Grand 7.4 Access Granded by Reader QUERY: STAKT DATE: 2/1/2021 12:0000 AM; BND DATE: 2/2/2021 12:0009 PM; READERS: DAXT-1.2:03-PDCK UP RM IN; DAXT-1.2:04-RETAINDOOM ENT ORGANIT 7.4 Access Granded by Reader QUERY: STAKT DATE: 2/1/2021 12:0000 AM; END DATE: 2/2/2021 12:00:00 PM; READERS: DAXT-1.2:03-PDCK UP RM IN; DAXT-1.2:04-RETAINDOOM ENT	Report Date: 3/2/2021, 1.57:42PM Eastern Standard Time	Event Details	1/28/2021 Access Granted 2/28/2021 Access Granted	341891952 410082878	38 39
Occument 7.4 Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXT-12:00-910X UP RM IN, DA	Report Date: 3/2/2021 1:57:42PM Bestorn Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datels Event Debels	2/28/2021 Access Granted	436192850	40
COCCUMENT A Account Street for Beautiful CLUERY: START DATE: 2/1/2021 12:00:00 AM; ENC DATE: 3/2/2021 13:00:59 PM; READERS: DAX7-1.2,03-PKX UP RM IN, DAX7-1.2,03-REARROOM ENT	Report Date: 3/1/2021 1:57:42PM Restern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	1/25/2021 Access Branced 2/25/2021 Access Granted	341890498 341919884	41 42
OWGARD 7.4 Access Granted by Render CUERT: START DATE: 2/1/2021 12:00:00 PAM; BVD DATE: 2/2/2021 11:50:50 PAM; READERS: DAXT-1.2:05-PICK UP RM IN, DAXT-1.2:04-BEANSHOOM ENT ORGAND 7.4 Access Granted by Render CUERT: START DATE: 2/1/2021 12:00:00 PAM; BVD DATE: 2/2/2021 11:50:50 PAM; READERS: DAXT-1.2:05-PICK UP RM IN, DAXT-1.2:04-BEANSHOOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/28/2021 Access Granted	134601775 251334739	43
On State of T.A. Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2021 11:59:59 PM; READERS: QAX7-12:00-PCX UP RMIN, DAX7-12:00-PCX UP RMIN, PCX UP RMIN, DAX7-12:00-PCX UP RMIN, PCX UP RMIN,	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	2/28/2021 Access Granted 2/28/2021 Access Granted	841906470	44
CONSTRUCT A Accord Structure by Bander CNERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:50:50 PM; READERS: DAX7-1.2.03-PKX UP RM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42FM Eastern Standard Time	Event Details	2/25/2021 Access Smited 2/25/2021 Access Smithed	341906622 436177463	46 47
DRIGHIET 7.4 Access Granned by Resider CRUENY: START DATE: 2/1/R0112-0000 AM; END DATE: 3/1/701111-50-50 PM; READERS: DAGG-1.2-G-PICK UP RM IN, DAYS-1.2-O-BREADRO-OM ENT ORGANIZATION OF A Access Granned by Resider CRUENY: START DATE: 2/1/202112-0000 AM; END DATE: 3/1/2021115-50-50 PM; READERS: DAGG-1.2-G-PICK UP RM IN, DAYS-1.2-O-BREADRO-OM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/28/2021 Access Granted	341925929	48
OnBused 7.4 Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2021 11:59:99 PM; READERS: DAX7-12:09-PICK UP RIA IN, DAX7-12:04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:43PM Eastern Standard Time	Event Details Event Details	7/28/2021 Access Granted 2/28/2021 Access Granted	216631#80 410429943	49 50
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ORGANI 7.4 Access General by Reader QUERY: STATI DATE: 2/1/2011 12:0000 AM; END DATE: 3/1/2011 11:00:00 PM; READERS: DAXT-12.03-PICK UP RM IN, DAXT-12.04-BRANDOOM ENT ORGANI 7.4 Access General by Reader QUERY: STATI DATE: 2/1/2011 11:00:00 AM; END DATE: 3/1/2011 11:00:00 PM; READERS: DAXT-12.03-PICK UP RM IN, DAXT-12.04-BRANDOOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detells	2/28/2021 Access Granted	289219346	53
2 TO A STATE OF THE PARTY OF TH	Report Date: 3/2/2021 157;42PM Sentern Standard Time Report Date: 3/2/2021 157;42PM Seatern Standard Time	Event Details Event Details	2/28/2021. Accept Granted 2/28/2021. Accept Granted	341891027 251308841	94 95
ORGUNS 7.4 Access Granted by Rander QUERY: START DATE: 2/1/2011 1250/00 AM; END DATE: 3/1/2011 1355/00 PM; READERS: CART-12.04-PICK UP RM IN, DATE-12.04-READERS: CART-12.04-PICK UP RM IN, DATE-12.04-READERS: DATE-12.04-PICK UP RM IN, DATE-12.04-PIC	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/28/2021 Access Granted 2/28/2021 Access Stratted	341926738 341884594	58 57
CinGuard 7.A. Access Brantad by Brader QUERY: START DATE: 2/3/2021 32:00:00 AM; END DATE: 3/2/2021 31:58:58 PM; READERS: DAXS-12.73-PICK UP RM RI, DAXY-12.04-8READEDOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/28/2021 Access Granted 2/28/2021 Access Granted	251309082	SB
Declared T.A. Revers Granded by Reader: CHERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2:05-PICK LIP RIM IN, DAX7-1.2:09-69/EARROOM ENT	Report Date: 3/2/2021 157/42PM Eastern Standard Time Report Date: 3/2/2021 157/42PM Eastern Standard Time	Event Details Event Details	2/28/2021 Access Granted 3/1/2021 Access Granted	289219546 43 6174753	59
On Guard 7.4 Assess Granted by Baseler OSSIGN 7.4 Recess Granted by Baseler OURSY: START DATE: 3/1/2011 120000 AM; BIO DATE: 3/1/2011 11:59:59 PM; BEADERS: DAX7-1.2.03-PICK UP RM NI, DAX7-1.2.04-BEADERS: DAX7-1.2.04-B	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datalis	3/1/2021 Access Granted	436177799	2
On Guerd 7.4 Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/2/2021 11:59:59 PM; READERS: DAID-12:09-PICK UP RM N, DAXY-12:00-BEAGROOM ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details Event Details	3/1/2021 Access Granted 9/1/2021 Access Granted	341892062 341890520	4
PARTICIPATE AND	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	3/1/2021 Access Granted 3/1/2021 Access Granted	341918556	5
OnStarrel 7.4 Access Greated by Reader QUERY: START DATE: 1/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:06:50 PM; READERS: DATY-1_203-PICK UP 9M IN; D	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	3/1/2021 Access Grented	436192047	7
Officiarit 7.4 Access Granted by Reader QUERY: STAUT DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DATO-1.2.03-PICK UP RM IN, DATO-1.2.04-BREADROOM ENT	Report Date: 3/2/2021. 1:57;42PM Eastern Standard Time Report Date: 3/2/2021. 1:57;42PM Eastern Standard Time	Event Details Event Details	3/1/2021 Access Granted 3/1/2021 Access Granted	436174401 341926827	9
CASCARD 2.4 Acres Street by Sender DUFRY: STAKE DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2.09-PICK UP RIA (N, DAX7-1.2.09-BITEAKROOM ENT	Report Date: 9/2/2021 1:57:42PM Eastern Standard Time Report Date: 9/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	3/1/2021 Access Granted 3/1/2021 Access Granted	25130781A 410440208	10 11
OnSigned 2.A. Access Granted by Render QUEENT: STATE DATE: 2/7/2001 12:30000 AMF, END DATE: 1/2/2001 11:35:59 PM, READERS: DAYY-1.2.09-PMCUP PM (M, DAYY-1.2.09-READERS) DATE: 1/2/2011 11:35:59 PM, READERS: DAYY-1.2.09-PMCUP PM (M, DAYY-1.2.09-READERS) DATE: 1/2/2011 11:35:59 PM, READERS: DAYY-1.2.09-PMCUP PM (M, DAYY-1.2.09-READERS) DATE: 1/2/2011 11:35:59 PM, READERS: DAYY-1.2.09-PMCUP PM (M, DAYY-1.2.09-READERS) DATE: 1/2/2011 11:35:59 PM, READERS: DAYY-1.2.09-PMCUP PM (M, DAYY-1.2.09-READERS) DATE: 1/2/2011 11:35:59 PMC (M, DAYY-1.2.09-PMCUP PM (M, DAYY-1.2.09-READERS) DATE: 1/2/2011 11:35:59 PMC (M, DAYY-1.2.09-PMCUP PM (M, DAYY-1.2.09-READERS) DATE: 1/2/2011 11:35:59 PMC (M, DAYY-1.2.09-PMCUP PM (M, DAYY-1.2.09-READERS) DAYY-1.2.09-READERS DATE: 1/2/2011 11:35:59 PMC (M, DAYY-1.2.09-READERS) DAYY-1.2.09-READERS DAYY-1.2.09-READER	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	1/1/2021 Access Granted	341924402	12
On Guard 7.4 Access Strandard by Reader CALETY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 8/2/2021 11:58:59 PM; READERS: DATG-1,2:03-PKCK UP RM RI, DAX7-1,2:04-BREADROOM ENT	Report Date: 3/2/2021 1:57:42PM Sentern Standard Time Report Date: 3/2/2021 1:57:42PM Sentern Standard Time	Event Details Event Details	3/1/2021 Acress Granted 3/1/2021 Acress Granted	436178476 436174665	14
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On Guard 7.A Access General by Reader QUERY: START DATE: 2/1/2001 12:00:00 AM; END DATE: 1/1/2011 11:59:39 PM; READERS; DMT-1_20.4FCK UP RM IN, DAXT-1_2.04-FICK UP RM IN, DAXT-1_2.04-	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	3/1/2021 Access Granted 3/1/2021 Access Granted	410422879 343891532	17 18
Opposited 2.4 Access Granted by Reeder Cupiffy: START DATE: \$1/\$2021.12:000 DAY, 900 DATE: \$1/\$2021.11:000 SP PM; READ/STE: DAZY-1.22:-PKE UP RIAN IN, DAZY-1.20:-REAGRISH MIN, DAZY-1.20:-REAGRISH	Report Date: 3/2/2021 1:57:42PM Gastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	3/1/2021 Access Grented	241924336	19
On Guard 7.A Access Synthetic by Rander QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:55:59 PM; READERS: SIAX7-1.2:09-PICK UP MAIN, CAX7-1.2:04-BIEARAGOOM ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details Event Details	3/1/2021 Access Granted 3/1/2021 Access Granted	341892084 341910988	26 21
OnSpired 2.4 Access Granted by Reader OUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/1/2021 11:59:99 PM; READERS: DAX2-12:00-PICK UP RM Rt. DAX2-12:00-PICK UP	Report Date: 3/2/2021 157:42PM Eastern Standard Time Report Date: 3/2/2021 157:42PM Eastern Standard Time	Event Details Event Details	3/1/2021 Access Granted 3/1/2021 Access Granted	341925218 341919622	23 23
CHRIGHER 7.A Access General by Render CULEUT: START DATE: 2/1/2011 15:00:00 AM; BRID DATE: 1/2/2011 15:00:00 PM; BRADERS; DADT-1.203-PRC UP RM IN; DADT-1.204-PREARROOM ENT CHRISTON ACCESS GENERAL BY READERS; DATT-1.204-PREARROOM ENT CHRISTON ACCESS GENERAL BY READERS GENERAL BY READERS GENERAL BY READERS GENERAL BY READERS GENERAL BY RE	Report Date: 3/2/2021 3:57:42FM Eastern Standard Three	Event Details	a/1/2021 Access Granted	486174979	24
CONSUMED 7.4 Access Streeted by Rander COLERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 13:59:59 PM; READERS: OAXT-1.2.09-PICK UP RM IN, DAX7-1.2.09-BEGARROCHA ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details		841925850 841889220	25 28
Critisand 7.4 Access Granted by Render OLERY: START DAYE: 2/1/2011/12:00:00 ANH; END DATE: 3/1/2011/11:00:00 PMF; SERVERS: DAYE-1_LID-FUX AF AND RE_DAYE-1_LID-FUX AF AND R	Report Date: 3/2/2021 1:57:42PM Seatom Standard Time	Event Details	a 9/1/2021 Access Granted	241908144 341891731	27 28
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ORGUNERY 7.4 Access Granted by Reeder QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXY-1.2:09-FICKUP RM IN, DAXY-1.2:00-REARRY/COM ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Sestem Standard Time	Event Detail: Event Details		498177507 410493578	30 31
THE COLUMN TO A SAME TO A	Report Data: 3/2/2021 1:57942PM Eastern Standard Time	Event Details	s 2/1/2021 Access Granted	241884594 410433578	32 23
On Guild 7.4 Acoses Grinned by Reader Curry: START ONTE: 2/1/2011 12:00:00 AM; END DATE: 3/1/2011 11:30:50 PM; READERS: DAYS-12.00-97 (UP RM IN, CAST-12.00-986ARROOM ENT ONTE: 3/1/2011 11:30:50 PM; READERS: DAYS-12.00-97 (UP RM IN, DAYS-12.00-986ARROOM ENT ONTE: 3/1/2011 11:30:50 PM; READERS: DAYS-12.00-97 (UP RM IN, DAYS-12.00-986ARROOM ENT ONTE: 3/1/2011 11:30:50 PM; READERS: DAYS-12.00-97 (UP RM IN, DAYS-12.00-986ARROOM ENT ONTE: 3/1/2011 11:30:50 PM; READERS: DAYS-12.00-97 (UP RM IN, DAYS-12.00-986ARROOM ENT ONTE: 3/1/2011 11:30:50 PM; READERS: DAYS-12.00-97 (UP RM IN, DAYS-12.00-986ARROOM ENT ONTE: 3/1/2011 11:30:50 PM; READERS: DAYS-12.00-97 (UP RM IN, DAYS-12.00-986ARROOM ENT ONTE: 3/1/2011 11:30:50 PM; READERS: DAYS-12.00-97 (UP RM IN, DAYS-12.00-986ARROOM ENT ONTE: 3/1/2011 11:30:50 PM; READERS: DAYS-12.00-97 (UP RM IN, DAYS-12.00-986ARROOM ENT ONTE: 3/1/2011 11:30:50 PM; READERS: DAYS-12.00-97 (UP RM IN, DAYS-12.00-986ARROOM ENT ONTE: 3/1/2011 11:30:50 PM; READERS: DAYS-12.00-97 (UP RM IN, DAYS-12.00-986ARROOM ENT ONTE: 3/1/2011 11:30:50 PM; READERS: DAYS-12.00-97 (UP RM IN, DAYS-12.00-986ARROOM ENT ONTE: 3/1/2011 11:30:50 PM; READERS: DAYS-12.00-97 (UP RM IN, DAYS-12.00-986ARROOM ENT ONTE: 3/1/2011 11:30:50 PM; READERS: DAYS-12.00-97 (UP RM IN, DAYS-12.00-986ARROOM ENT ONTE: 3/1/2011 11:30:50 PM; READERS: DAYS-12.00-97 (UP RM IN, DAYS-12.00-986ARROOM ENT ONTE: 3/1/2011 11:30:50 PM; READERS: DAYS-12.00-97 (UP RM IN, DAYS-12.00-986ARROOM ENT ONTE: 3/1/2011 11:30:50 PM; READERS: DAYS-12.00-97 (UP RM IN, DAYS-12.00-986ARROOM ENT ONTE: 3/1/2011 11:30:50 PM; READERS: DAYS-12.00-97 (UP RM IN, DAYS-12.00-986ARROOM ENT ONTE: 3/1/2011 11:30:50 PM; READERS: DAYS-12.00-97 (UP RM IN, DAYS-12.00-986ARROOM ENT ONTE: 3/1/2011 11:30:50 PM; READERS: DAYS-12.00-97 (UP RM IN, DAYS-12.00-986ARROOM ENT ONTE: 3/1/2011 11:30:50 PM; READERS: DAYS-12.00-97 (UP RM IN, DAYS-12.00-986ARROOM ENT ONTE: 3/1/2011 11:30:50 PM; READERS: DAYS-12.00-97 (UP RM IN, DAYS-12.00-986ARROOM ENT ONTE: 3/1/2011 11:30:50 PM; READERS: DAYS-12.00-9	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/1/2021 1:57:42PM Eastern Standard Time	Event Detail	3/1/2021 Access Branted	341926738	84
Origuerd 7.4 Access Grented by Reader QUERT: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/2/2021 11:50:99 PM; READERS: DAX7-1,2.03-PICK UP RM IN, DAX7-1,2.04-REARINGOM ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Detail Event Detail	s 3/1/2021. Access Greented	341925284 341888757	25 26
CONTROL OF A ANNUAL PROJECT IN SERVE STATE DATE: 2/1/2021 12:00:00 AM: END DATE: 3/2/2021 11:58:59 PM; READERS: DAXY-1.2:03-PICY LIP RIM IN, DAXY-1.2:04-89/EARROUM ENT	Report Date: 2/2/2021 1:57:42PM Eastern Standard Time	Event Detrill	k 3/1/2021 Access Granted	253259810 251303688	37 85
On Guerd 7.4 Access Green to Up Resider CLUENT: START DATE: 2/1/2011 12:00:00 AM; DED DATE: 1/2/2012 11:15:559 PM; READERS: DMY-1_2:05-FICE UP WARNER, DAYN-1_2:06-BEARGOOM BENT DATE: 2/1/2011 12:00:00 AM; DED DATE: 1/2/2012 11:55:559 PM; READERS: DMY-1_2:05-FICE UP ADVIN-1_2:06-BEARGOOM BENT DATE: 2/1/2011 12:00:00 AM; DED DATE: 1/2/2012 11:55:559 PM; READERS: DMY-1_2:06-FICE UP ADVIN-1_2:06-BEARGOOM BENT DATE: 2/1/2011 12:00:00 AM; DED DATE: 1/2/2012 11:55:559 PM; READERS: DMY-1_2:06-FICE UP ADVIN-1_2:06-BEARGOOM BENT DATE: 1/2/2012 11:55:559 PM; READERS: DMY-1_2:06-FICE UP ADVIN-1_2:06-BEARGOOM BENT DATE: 1/2/2012 11:55:559 PM; READERS: DMY-1_2:06-FICE UP ADVIN-1_2:06-BEARGOOM BENT DATE: 1/2/2012 11:55:559 PM; READERS: DMY-1_2:06-FICE UP ADVIN-1_2:06-FICE UP ADVIN-1_2:06-FI	Raport Date: 3/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Detail	s 3/1/2021 Access Branted	172905979	35
Griduard 2.4 Access Granted by Reader QUERY: START DATE: 2/1/2021 22:00:00 AM; END DATE: 2/2/2021 11:50:59 FM; READERS: DAGY-12_00-PICK UP RM PR, DAGY-12_00-READERSOND BY	Report Date: 3/2/2021 1:57:42PM Seatern Standard Time Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Detail		251309698 436192950	40
On Grand 7.4 Access Granted by Region QUERY: START DATE: 1/1/2011 12:00:00 AM; END DATE: 1/1/2011 11:09:59 PM; READERS: DATE-1.2.03-PKX UP RM IN, DATE-1.2.04-PREADROOM ENT DH-Gland 7.4 Access Granted by Region QUERY: START DATE: 1/1/2021 12:00:00 AM; END DATE: 3/1/2021 11:59:59 PM; REQUERS: DATE-1.2.03-PKX UP RM IN, DATE-1.2.04-PREADROOM ENT	Report Date; 3/2/2021 1:57:42PM Eastern Standard Time	Event Detel	ls 3/1/2021 Access Granted	251303688 251302698	42 43
CINEBURI 7.4. Access General by Rander CURET: STANT DATE: 2/1/2011 12:00:00 ANY: BND DATE: 3/1/2011 11:09:09 WY, READISK: DATE-1_2:00-PICK UP RAINE, DATE-1_	Report Date: 9/2/2021 1:57:42PM Eastern Standard Time Report Date: 3/2/2021 1:57:42PM Fastern Standard Time	Event Detail	is 3/1/2021 Access Grented	251309052	44
On Guerd 7.4 Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS; DAKY-1,2:00-PKK UP RNA IN, DACY-1,2:04-READERDOM ENT	Report Date: 3/2/2021 1:57:42PM Sestem Standard Time Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Evant Datal		251334981 436192950	45 45
OnBoard 7.A. Access Senated by Reside: QUERY: START DATE: 2/1/2001 12:00:00 AM; EMD DATE: 3/1/2001 15:959 PM; READERS: DATO-1.2:01-98:00 PM; DATO-1.2:01-98:00 PM; DATO-1.2:01-98:00 PM; DATO-1.2:01-98:00 PM; READERS: D	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Fivent Detail	fs 3/1/2021 Access Granted	134691775 251334739	47 48
OnGuard 7.4 Access Granuld by Reader CUERY: START DATE: 2/1/2011 23:00XI AM; BND DATE: 3/2/2011 13:595 PM; READERS: DATA-1.2.09-RCK UP RM IB, DATA-1.1.09-READOOM BNT CONTROL OF THE CONTR	Report Date: a/z/2021. 1:57:42PM Eastern Standard Time Report Date: 2/z/2021. 1:57:42PM Seatern Standard Time		ls. 3/1/2021. Access Granted	341919864	49
CONTROL OF THE PARTY OF THE PAR	Report Date: 2/2/2021 1:57:42FM Eastern Standard Tim Report Date: 3/2/2021 1:57:42FM Eastern Standard Tim	Event Detail		213611820 341906822	50 51
Onguerd 7.4 Access General by Reader CUERY: STATE DATE: \$2/1/2021 12:00:00 AM; END DATE: \$/1/2021 11:20:00 PM; READERS: DAX7-1.2.04-PICK UP RM IN, DAX7-1.2.04-REAKROOM ENT ONGWERT 7.4 Access General by Reader CUERY: STATE DATE: \$2/1/2021 12:00:00 AM; END DATE: \$1/2/2021 11:20:00 PM; READERS: DAX7-1.2.04-PICK UP RM IN, DAX7-1.2.04-REAKROOM ENT	Report Date: 9/2/2025 1:57:42PM Eastern Standard Tim	e Event Deta	lls 3/1/2021 Access Grantes	289218731 341925913	52 5a
On Quarted 7.4 Access Granted by Reader QUERY: START DATE: 2/1/IEE1 12:00:00 AM; END DATE: 2/2/2021 11:59:39 PM; READERS: DAX7-3-2:03-PICK UP RM IN, DAX7-3-1:04-BREADROOM BY	Report Date: 3/2/2021 1:57:42PM Gustern Standard Tire Report Date: 3/2/2021 1:57:42PM Eastern Standard Tire	 Event Deta 	fls 3/1/2021 Accuss Greented	4340,90703	54
OnGuard 7.4 Access Granted by Reader QUERY: START DATE: 2/1/2021 12:00:00 AM; END QATE: 3/1/2021 11:59:59 PM; READERS: DADY-12:00-PICK UP RIAIN, DAXY-12:04-BREANROOM ENT	Report Date: 3/2/2021 2:57:42PM Eastern Standard Tim Report Date: 3/2/2021 1:57:42PM Eastern Standard Tim	Event Date			.95 58
On Guard 7.4 Access Grented by Reader OLESY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:50:09 PM; REVDERS: DAXT-1.2.03-PICX UP RM IN, DAXT-1.2.04-REGARDOM ENT ORGANIZAR Access Segregal by Reader CUGSY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: 0AXT-1.2.03-PICX UP RM IN, DAXT-1.2.03-PICX UP RM IN, D	Report Date: 3/2/2021 1:57:42PM Eastern Standard Tim				57

			Event Details	3/1/2021 Access Granted	541891027 58
On Buard 7.4 Access Granted by Reader	QUIENY: START DATE: 2/1/2021 12/SEXXX AM; END BATE: 9/2/2021 11:59:59 PM; READERS: DAXY-L2:05-PKX UP RM:N, DAXY-L2:04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	2/1/2021 Access Granted	341906734 59
On Guard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:55 PM; READERS: DAX?-1.2:03-PICK UP RM IN, DAX?-1.2:04-BREADRQOM ENT	Report Oute: 3/2/2021 1:57:42PM Eastern Standard Time Report Oute: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	3/1/2021 Access Granted	289219546 60
On Guerd 7.4 Access Grented by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:58:59 PM; READERS: DATE-12:05-PKK UP RM IN, DATE-12:04-BREADRICHM ENT		Event Details	3/1/2021 Access Granted	2892223-86 61
GnSuard 7.4 Access Greented by Reader	QUERT; START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX?-1.2.09-PKX UP RM IN, DAX?-1.2.09-EREADROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time		3/1/2021 Access Granted	241891665 1
OnGuard 7.4 Access Grented by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAID-1.2.03-PMCX UF RM IN, DAID-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details		341892082 2
Chilliand 7.4 Access Granted by Reader	QUERY; START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:50:59 PM; READERS: 0AX7-1.2.09-PICK UP RM UN, DATG-1.2.09-BREACROCK ENT	Report Date: 3/2/2021, 1:57/42PM Eartern Standard Time	Event Details	3/2/2021. Access Granted 3/2/2021. Access Granted	341890454 5
Options 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/2/2021 11:59:59 PM; READERS: DAXT-1.2.03-PICK UP RIM IN, DAXT-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details Event Details	3/2/2021 Access Granted	341890520 4
OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:50:59 PM; READERS: DAXY-L2.09-PICE UP RM IN, DAXY-L2.09-BREADROOM ENT	Report Date: 3/2/2021 1:57:12PM Eastern Standard Time	Event Details	3/2/2021 Access Grented	341913982 5
DriGuerd 7.4 Access Granted by Ressier	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.03-REARROOM ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details	3/2/2021 Access Granted	341919556 6
OnGuard 7.4 Acress Granted by Reader	QUERY: START DATE: 1/1/2021 12:00:00 AM; END DATE: 1/2/2021 11:59:59 PM; READERS: DATF-12:09-PICK UP RM IN, DATF-12:04-BREAKRODM ENT	Report Dete: 3/2/2021 1:57:42PM Restern Standard Time	Event Detells	3/2/2021 Access Granted	341890233 7
OnGuard 7.4 Access Scantad by Reader	QUERY: \$7ART DATE: 2/1/2021 12:00:00 AM; SND DATE: 8/2/2021 11:58:59 PM; READERS: DAX7-1.2.01-PICK UP RM IN, DAX7-1.2.01-BREAKRODM ENT	Report Date: 3/2/2021 1:57;12PM Fastern Standard Time	Event Details	3/2/2021 Access Grented	496192047 8
On Staard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1-2:03-PICK UP RM UN DAX7-1-2:04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Enstern Standard Time	Event Details	3/3/2021 Access Granted	341874336 9
OnGuard 7.4 Access Granted by Asader	QUERY: STARY DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 31:58:59 PM; READERS: DAX7-1,2:04-PICKUP RM IN, DAX7-1,2:04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:49PM Eastern Standard Time Report Date: 3/2/1021 1:57:49PM Eastern Standard Time	Event Datells	3/2/2021 Access Granted	341913079 10
DeGused 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX9-1-2.09-PICK UP RM WL DAX9-1-2.09-DRGAKROOM ENT		Event Details	3/2/2021 Access Granted	341910988 11
OnGuerd 7.A Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.2.00-PICK UP RIM IN, DAX7-1.2.00 BREADERS	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Octalis	#/2/2021. Access Granted	436174401 12
OrrGuard 7.4 Access Granted by Reader	QUERY: START DATE: 7/1/2021 12:00:00 AM; END DATE: 9/2/2021 11:59:59 PM; READERS: DAXY-1.2.09-PICK UP RIM IN, DAXY-1.2.09-BRATEDOMA ENT	Report Date: 5/2/2021 1:57:42PM Sestem Standard Time Report Date: 2/2/2021 1:57:42PM Sestem Standard Time	Event Details	3/2/2021 Access Stented	841919644 13
OpGuard 7,4 Access Granted by Reader	QUERY: START DATE: 1/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXT-1.2.03-PICK UP RIM IN, DAXT-1.2.03-BREAKBOOM ENT	Report Date: 2/2/2021 1:57/42PM Eastern Standard Time	Event Details	3/2/2021 Access Grented	341924402 14
On Shand 7.4 Access Granted by Reader	QUERY: START DATE: 3/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:99:99 PM; HEADERS: DAX7-1.2.03-PICK UP RM IN, DAX7-1.2.04-REAKROOM ENT	Report Date: 3/2/2021 1:57/42PM Eastern Standard Time Report Date: 3/2/2021 1:57/42PM Eastern Standard Time	Event Details	3/2/2021 Access Granted	41.044(2)09 15
OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:54:59 PM; READERS: DAX7-1.2.01-91CK UP RIM UK, DAX7	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	3/2/2021 Access Granted	436178432 16
OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:50 PM; READERS: DAX7-1-2.03-PICK UP RM IN, DAX7-1-2.04-PREADROOM ENT	Report Date: 3/2/2021 1:57/42PM Eastern Standard Time	Event Details	3/1/2021 Access Granted	341888757 17
On Guard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1.105-PICK UP RM W, DAX7-1.204-BREAKROOM ENT	Report Date: 3/2/2021 1:57/42PM Eastern Standard Time	Event Details	3/1/2021 Access Granted	436178476 18
On Guard 7.A Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAIZ-1,2:0: FICK UP RM IN, OAXZ-1,2:0-BREAKROOM ENT	Report Date: 3/2/2021 1:57/42PM Eastern Standard Time	Erent Datails	3/2/2021 Access Brented	436174665 19
On Guard 7.4 Accress Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 9/2/2021 11:59:59 PM; READERS: DAXY-1-2:03-PICK UP RM IM, DAXY-1-2:01-BRSAKROOM ENT	Report Date: 3/2/2021 1:57929 M Eastern Standard Time	Event Details	3/2/2021 Access Granted	341892084 20
OnGuerd 7.4 Actes Grented by Reader	QUERY: START DATE: 2/1/202112.50503 AM; END DATE: 3/1/202111.56:59 PM; READERS: DAXY-1.2.09-FICK UP RISH IN, DAXY-1.2.09-FICK UP RISH IN DAXY-1.2.09-FICK UP RI	Report Date: 3/2/2021 1:57:429M Sestem Standard Time	Event Details	3/2/2021 Access Granted	341891593 21
OnGuard 7.4 Acress Grented by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: QAX7-12:00-PICK UP RIM IN, DAX7-12:04-88-644000M ENT	Report Date: 3/2/2021 2:57:42PM Eastern Standard Time	Event Datella	1/2/2021 Access Granted	341926627 22
OnGuerd 7.4 Access Grented by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXS-12.03-PICK VP RM IN, DAXS-1.2.04-READROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	3/2/2021 Access Grented	341919622 23
On@uerd 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/3021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAXY-1.2.03-PICK UP RAM IN, DAXY-1.2.04-READROOM ENT	Report Date: 3/2/2021 1:57/42PM Eastern Standard Time	Event Details	3/2/2021 Access Granted	341925319 24
OnGuard 7.4 Access Granted by Reader	QUERY: \$7ART DATE: 2/1/2021 12:00:00 AM; SND DATE: 3/2/2021 11:59:59 PM; READERS: DAXY-1.2:02-NCK UF RM IN, DAXY-1.2:04-BEAGROOM ENT	Report Cate: 9/2/2021 1:57:42PM Eastern Standard Title	Event Details	3/2/2021 Access Granted	343922904 25
OnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 2/2/2021 11:59:59 PM; READERS: DARF-1.2/07-PCX UP RM IN, DAX7-1.2/04-BREAXROOM ENT QUERY; START DATE: 2/1/2021 12:00:00 AM; END DATE: 1/2/2021 11:59:59 PM; READERS: DAX7-1.2/07-PCX UP RM IN, DAX7-1.2/04-BREAXROOM ENT	Apport Date: 8/2/2021 1:57:42PM Eastern Stunderd Time	Event Details	2/2/2021 Access Grented	341892767 26
OnGuzze 7.4 Access Granted by Reader		Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Outels	3/2/2021 Access Bounted	3418BGSSB 27
On Stand 7.4 Access Granted by Resder	QUERY: START (147E: 2/1/2021 12:00:00 AM; END DATE: 9/2/2021 11:59:59 PM; READERS: DAX7-12:09-PKX UP RAFIN, DAX7-12:04-DREAKIDOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	3/2/2021 Access Granted	341925218 28
On Guard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:0000 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-12.06-PKX UF RM IN, DAC7-12.06-REAGROOM ENT CHERY: START DATE: 2/1/2021 12:0000 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-12.06-PKX UF RM IN, DAX7-12.06-REAGROOM ENT	Report Date: 3/2/2021 1:57/42PM Eastern Standard Time	Event Details	3/2/2021 Access Granted	341922570 29
DeGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 8/2/2021 11:59:59 PM; READERS: DATO-1.2:09-PICK OF WINTIN, DATO-1.2:04-BREADERS DATO-1.2:09-PICK OF WINTIN, DATO-1.2:04-PICK OF WINTIN, DATO	Report Date: 3/2/2021 1:57:62PM Seatern Standard Time	Event Details	3/2/2021 Access Granted	436174379 30
UnGuard 7.4 Access Granted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAV7-12:03-PICK UP NM IN, DAV7-12:04-9REAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	3/2/2021 Access Granted	496192950 31
On Guard 7.4 Azensa Granted by Render	CHEMY; START DATE: \$41,200.1 12,000.00 AM; END DATE: \$42,200.1 11.59.59 PM; READERS: DAX1-12,01-91CK UP RM IN, DAX7-12,00-BREARROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datalla	3/2/2021 Access Granted	235838646 32
On Buard 7.4 Access Startled by Render	QUERY: \$1.04T DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DAX7-1,2,03-910X UP RM IN, DAX7-1.2.04-BREAKROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Datab	3/2/2021 Access Spanted	436188698 33
OnBused 7.4 Access Smarted by Reader	QUERT: STATE DATE: 2/1/2013 12:0000 AM; END DATE: 3/2/2013 11:0000 PM; READERS: DATF-12:01-PICK UP RM NI, DAXF-12:00-REARCOM ENT	Report Date: 9/2/2021, 1:57:42PM Eastern Standard Time	Event Details	3/2/2021. Accres Branted	4361,74709 34
OnGuard 7.4 Access Granted by Reader	CUERY, START DATE: 2/1/2021 12:0000 AM, END DATE: 2/2/2021 11:56:59 PM, READERS: DATE-1.201-FICK UP RM M, DATE-1.201-REARROOM ENT	Report Date: 9/2/2021, 1:57:42PM Eastern Standard Time	Boot Details	8/2/2021 Access Grented	341922970 35
OnGuard 7.4 Access Granted by Reader	CHERT, START DATE: 2/1/2021 12:0000 AM; END DATE: 3/2/2021 13:5599 PM; READERS: DAXY-12:00-FICK OF RM IN, DAXY-12:00-FICK	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	3/2/2021 Access Granted	48.6192950 36
On Suerd 7.4 Access Grented by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:59 PM; READERS: DATO-12:09-FIX UP RM IN, DATO-12:00-REARROOM ENT	Report Date: 3/1/2021 1:57:43PM Eastern Standard Time	Event Details	3/2/2021 Access Granted	251309052 37
On Suard 7,4 Access Granted by Reader	QUERY: START DATE: 27/2/20112:00:00 AM; END DATE: 3/2/20211159:59 FM; READERS; DADT-1.2.09-FICK UP RM IN, DATF-1.2.04-BREADROOM ENT	Report Date: 3/2/2021 1:57:42PM Eastern Standard Tirre	Event Details	3/2/2021 Appens Granted	341922970 35
OnGuard 7.4 Access Stranted by Reader	QUERY: START DATE: 2/1/2021 12:00:00 AM; END DATE: 3/2/2021 11:59:69 PM; REDUCKS: DAXC-12:00-PICK UP RM IN, DAXT-12:00-BREAKROUND ENT	Report Date: 3/2/2021 1:57:12PM Eastern Standard Time	Event Detells	3/2/2021 Appear Granted	134591775 39
OnGuard 7.4 Access Granted by Reader OnGuard 7.4 Access Granted by Reader	QUERY: STATE DATE: 2/1/2011 12/00/00 AM; END DATE: 3/1/2021 11:59:59 PM; READERS: DAYS-1.2.00-PICK UF RM IN, DAYS-1.2.04-BREAGROOM ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Detalls	3/2/2021 Access Granted	410429965 40
	QUEST: START DATE: 2/1/2021 12:0000 AM; END DATE: 2/2/2021 11:59:55 PM; READERS: DAXT-12:00+PICK UF RM IN, DAXT-12:00+REARROOM ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Detalls	2/2/2021 Access Granted	232763722 41
OnGuerd 7.4 Access Granted by Reader OnStant 7.4 Access Granted by Reader	QUERY, START DATE: 2/1/201120000 AM; END DATE: 3/2/201115959 PM; READERS: DAX/-1_03-PICKUR RM IN, DAX/-1_204-BREAKHOOM ENT	Report Date: 3/2/2021, 1:57:42PM Eastern Standard Time	Event Details	3/2/2021 Access Grented	2892 1873 1 42
On Sunne 7.4 Access Granted by Reader On Suard 7.4 Access Granted by Reader	QUERY: START DATE: 3/1/2021 12:00:00 AM: END DATE: 3/1/1021 13:55 PM; GENERAL DATE: 3/1/1021 13:55	Report Date: 3/2/2021 1:57:42PM Eastern Standard Time	Event Details	5/2/2021 Access Granted	251207818 43
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PARTIAL EAST ELEVATION



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TENANT IMPROVEMENT FREEWAY BUSINESS CENTER 2677 E ALESSANDRO BLVD RIVERSIDE, CA 92508

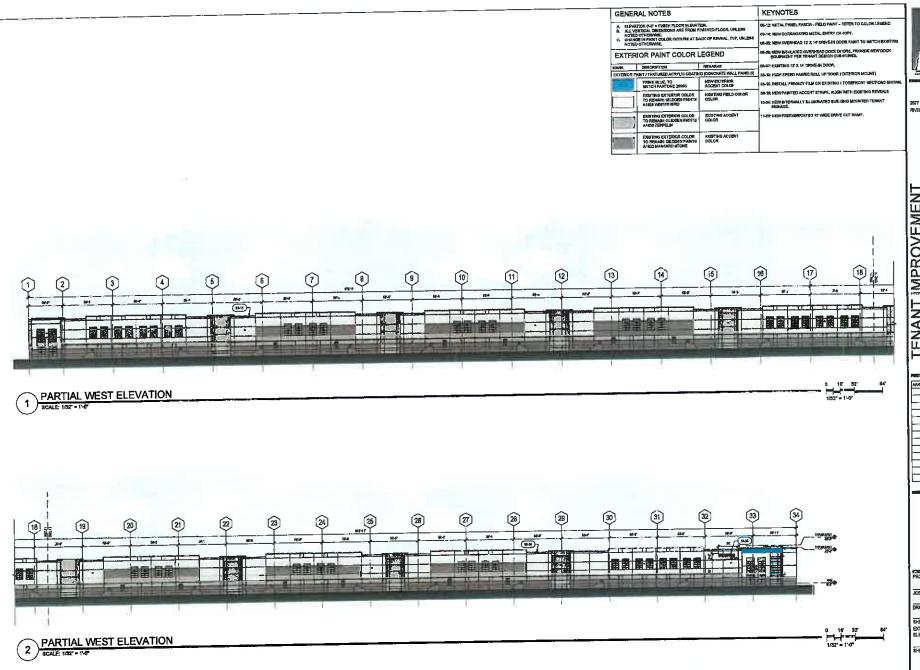


PROJECT STATUS

JOB NUMBER 20-591 0-01 CHECKED BY DRAWN BY BSJ SHEET TITLE

EXTERIOR ELEVATIONS

SHEET NUMBER A2.10





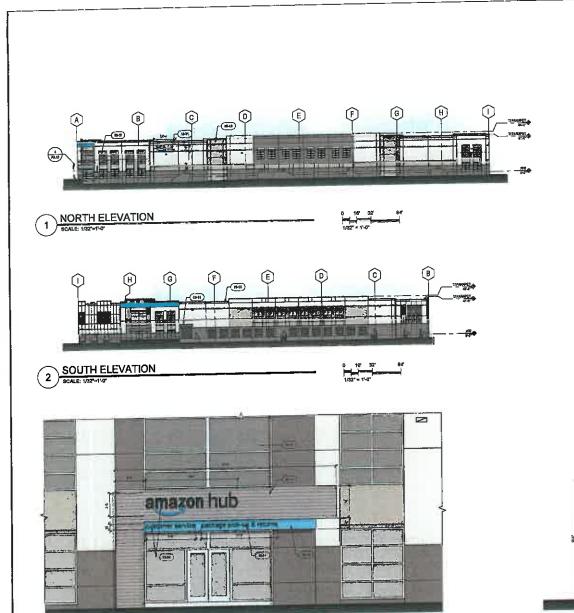
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TENANT IMPROVEMENT FREEWAY BUSINESS CENTER 2677 E ALESSANDRO BLVD RIVERSIDE, CA 92508

PROJECT STATUS JOB NUMBER

BSJ SHEET TITLE EXTERIOR ELEVATIONS

SHEET NUMBER A2.11



EAST ENLARGED ELEVATION

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GENERAL NOTES

EXTERIOR PAINT COLOR LEGEND

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QUIDDEN PAINTS	EXISTING ACCENT
A1830 ZEPPELIN	COLOR TO REMAIN
GLEDEN PAINTS	EXISTING ACCIENT
A1832 MANGARD STORE	COLOR TO REMAIN
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KEYNOTES

98-12: NETAL PANEL PASCIA - FIELD PAINT - ILEFER TO COLOR LEGENC

10-04: BUILDING MOUNTED TENANT GROWAGE

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TENANT IMPROVEMENT FREEWAY BUSINESS CENTER 2877 E ALESSANDRO BLVD RIVERSIDE, CA 92508

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SHEET TITLE EXTERIOR ELEVATIONS

SHEET NUMBER A2.12

NORTH ENLARGED ELEVATION

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AMAZON LOGISTICS OPERATIONAL NARRATIVE

Operational Overview

Amazon Logistics ("AMZL") is a service that fulfills customer orders. AMZL specializes in "last mile" delivery of customer orders from delivery stations. Packages are shipped to AMZL delivery stations from Amazon fulfillment and sortation centers. Packages arrive from line haul trucks, are sorted based on zip codes and loaded into delivery vans operated by delivery service partners ("DSP").

Delivery stations operate 24/7, with most of the sortation activity done early in the morning when the line haul trucks arrive with customer packages. At our proposed Riverside County, California facility, AMZL line haul trucks will be delivering packages to the delivery station each day, primarily between the hours of 10:00 PM to 8:00 AM. Associates sort the packages by routes, place the packages onto movable racks and load the packages into the delivery vans primarily between 12:30 AM and 11:00 AM with Amazon associates entering and departing between those times. Additionally, there will be managers supervising the DSP operations, arriving between 8:00 AM and 11:00 AM and departing between 7:00 PM and 10:00 PM.

The first "wave" of DSP drivers arrive at the delivery station at approximately 10:00 AM. At this location, DSP drivers will typically park their personal vehicles onsite and pick up their delivery vans. To keep site operations efficient and secure, DSP drivers are instructed to arrive only just before their scheduled shift time. Once at the delivery station within their delivery van, DSP drivers take position in a vehicle queue that spaces out the group and secures their movement for optimal organization. In a synchronized manner, the drivers load their individual delivery vans within sequenced, segmented groups and depart to deliver packages directly to customers. Each delivery wave takes about 20-30 minutes to load and depart. During this time, the van positions in the facility of earlier scheduled driver groups provide a physical buffer to the entry of later arriving groups. This standard operating procedure acts as an added measure to ensure proper occupancy of the building at this location. As a wave of DSP drivers prepare to depart, a new wave of DSP drivers queue and prepare to load their delivery van. The last wave of DSP drivers depart the delivery station around 1:00 PM. DSP van drivers typically complete one round trip per day.

After DSP drivers complete their routes, they return to the delivery station with any packages that may have been non-deliverable. They typically return to the facility between 7:00 PM to 10:00 PM or after rush hour traffic. After proper checkout and release, the DSP drivers typically park their delivery vans onsite, and leave using a personal vehicle or public transport.

After departure of the last wave of delivery vehicles, delivery station associates prepare the delivery station for the next day's packages.

Employment

Amazon strives to provide job local opportunities for the communities and the residents that it serves. Historically, Amazon has conducted local hiring fairs and has partnered with local workforce development organizations to market new Amazon employment opportunities to residents who live in and around its facilities. We have found that building local partnerships is key to a successful local hiring initiative. Our growth is the result of an outstanding workforce, strong local support, and incredible customers. Our



associates and customers in this region are also your residents, and we want to ensure we are being good neighbors.

Delivery Stations create hundreds of mostly full-time and part-time jobs. Amazon provides all associates starting at least \$15 per hour in wages, and a variety of benefits packages from day one. Wages in the California are currently trending significantly higher than \$15. All full time employees receive benefits including medical, dental and vision insurance, 401k match, and life and disability insurance. Part-time opportunities are great for parents seeking flexibility in schedule, college students, those seeking second jobs, and retirees. For the proposed delivery station, we are planning 70% full-time jobs.

Four types of jobs are available in delivery stations:

- 1. <u>Sortation</u> These associates are directly employed by Amazon. They help with sorting packages inside the delivery station.
- 2. <u>Delivery Service Partners (DSP)</u> DSPs are entrepreneurs who have launched their own small business delivering packages on behalf of Amazon. DSPs operate out of Amazon's delivery stations and employ delivery van drivers who deliver Amazon packages. They adhere to the minimum \$15 per hour wage requirement for their employees. After 30 days of hire, DSPs are eligible for benefits.
- 3. <u>Managers</u> Managers are employed by Amazon for managing the sortation process, and by DSP owners for managing the delivery process.
- 4. <u>Flex Drivers</u> Flex drivers are independent contractors who own their own vehicles and create their own schedules delivering packages on behalf of Amazon. Amazon Flex drivers can earn more than between \$18 and \$22 per hour. However, these positions do not apply at this facility.

Environmental Plan

In alignment with the County's policies to promote healthy and sustainable communities, Amazon is committed to being a leader in sustainability. In support of The Climate Pledge commitment to be net zero carbon by 2040, 10,000 Amazon custom electric delivery vehicles will be on the road delivering to customers worldwide as early as 2022 and all 100,000 by 2030. Amazon has been busy engaging several small startups and vehicle manufacturers to pursue multiple simultaneous paths.

As part of Amazon's commitment to power all operations with 100% renewable energy by 2030, the Sustainability team has developed an on-site solar strategy for rapid deployment. Amazon leverages a financing mechanism called a Power Purchase Agreement (PPA) to execute deals and deploy solar. Sustainability has negotiated Master Service Agreements (MSAs) with industry leading solar developers to provide turn-key solar projects to Amazon. Amazon will evaluate solar panel deployment at this location in-line with any unique regulatory requirements.

Installation of rooftop solar on an Amazon building can drive financial savings, reduce grid energy consumption, and decrease carbon emissions. As of year-end 2019, Amazon has over 90 on-site solar projects installed and generating electricity in North America, The Middle East, and Africa, totaling more than 130 Megawatts. Amazon has over 40 operational on-site solar PV systems in the United States and with a focus on Fulfillment and Sort Centers.



Transportation Demand Management (TDM)

There are several standard TDM measures Amazon is taking for its delivery stations:

- Pre-Tax Transit Pass Benefits All AMZL Associates are able to use WageWorks to purchase transit passes or other approved commuter expenses pre-tax.
- 2. <u>Carpool and Vanpool Ride-Matching Services</u> AMZL promotes both Waze Carpool and local carpool/vanpool ride-matching services wherever available.
- 3. <u>Bicycle Parking</u> Each Delivery Station offers employee bike racks and day use lockers.
- 4. <u>Guaranteed Ride Home (GRH) Program</u> AMZL offers a free GRH program for employees arriving to work by carpool, vanpool, or transit and need to leave work early or are unable to use their normal commute accommodations.
- 5. <u>Designated Employee Transportation Coordinator (ETC)</u> Each Delivery Station has a designated ETC to answer employee commute related questions and advertise any available commuter benefits.

Community Engagement

Our associates in this region are also your residents and we want to ensure we are being good neighbors. To assist in this effort, Amazon has created a new community engagement team to work specifically with communities that have new delivery stations to forge new partnerships. Amazon offers several programs that will impact the Community in various ways. They include:

- O Amazon Future Engineer Amazon Future Engineer is a four-part, childhood-to-career program aimed at inspiring and educating hundreds of thousands of students from underrepresented and underserved communities each year to try computer science and coding. The program has an online application process that can be found at the following web address: https://www.amazonfutureengineer.com/
- School donations/giving Amazon in the Community Programs makes community-based donations to a variety of programs and organizations throughout the United States.

It is our desire to have such a positive impact that you feel like this community has thrived as a result of our presence and associates in our delivery stations and drivers on the road are members of this community and share this same passion.

Workplace Conditions

Ensuring the safety of associates, drivers, and members of the communities in which we operate is our number one priority. Operational meetings, new hire orientation, process training and new process development begin with safety and have safety metrics and audits integrated within each program.



Training is constant, both in making sure employees know how best to work with the technology in the facility and also how to prevent injuries. While any serious incident is one too many, we learn and improve our programs working to prevent future incidents.

Our Delivery Stations are typical for the logistics industry where due to loading of vehicles, associates are often inside and outside as part of their shift. We closely monitor the temperatures in the stations to ensure a safe work environment and we also have industrial-grade fans, cooling mists, an abundance of water and other measures to ensure the safety of those at the site.

COVID-19

We are in unprecedented times and are taking exceptional measures to ensure the safety of our associates and communities during the COVID crisis, so that we can bring essential goods to those in need.

Amazon's facilities follow all health and safety protocols including social distancing guidelines. Across operations, Amazon has invested more than \$800 million dollars in the first half of this year on safety measures like temperature checks, masks, enhanced cleaning, gloves, sanitation stations, and on-site testing, to name a few.

We've made over 150 process updates to help protect employees, have implemented enhanced cleaning and social distancing measures, and we're piloting new efforts like using disinfectant fog in our New York fulfillment center. We've already had 175,000 new hires start new positions opening up to help meet customer demand and assist existing employees fulfilling orders for essential products.

Since the beginning of COVID-19, Amazon has made personal protective gear such as masks for our hourly employees and implemented temperature checks across our operations worldwide. Leadership at all levels in Amazon are working day and night every day continuously monitoring the effectiveness of the changes we made in the network and looking for opportunities for continuous improvement. We are also looking for ways to leverage technology to help us audit social distance compliance. We know this new normal is a big change for everyone and recognize the importance of inspecting our processes and looking for ways to make it easier to comply and ensure safety. Our facility's design, intended to make a more social distance-friendly environment, is an additional program element in alignment with occupancy requirements at this location. However, irrespective of this global health emergency, both at project launch and future peak operations, this facility will comply with all required building regulations.



Transportation Engineering Services

2677 E. ALESSANDRO BLVD RIVERSIDE, CA 92553 DFX5 - DESIGN INTENT AND SCOPE

Date: December 28, 2020

Project Data:

Developed Site: 39.23 AC Building Size: 706,081 SF

Office TI: +/-18,715 SF (Proposed)

Existing TI: 1st Floor Office: 3,915 SF / 2nd Floor Mezzanine: 3,000 SF

Summary:

This site consists of recently completed construction including site improvements and adjacent public improvements previously approved by all AHJ per plot plan 14-02 including Resolution JPA# 18-03. The existing conditions include a new 706,081 sf concrete tilt-up building shell; a 3,915 sf speculative office build out, a 3,000 sf mezzanine, two single use unisex restrooms in warehouse and associated site improvements including but not limited to; site lighting, landscaping, parking striping, and ADA parking/accessible routes. Building is currently served with 4000A single service permanent power.

The proposed scope of work is a first-generation tenant improvement project which will occupy the entire building. Approximately 1/3 of the floor area will be dedicated to interior van parking and van loading area. Vans will park in this area overnight, but the parking area will be typically empty during the day when the vans are on their daily delivery routes.

215,000 sf of floor area is dedicated to package sortation and material handling operations. Packages arriving at this facility are prepackaged in cardboard boxes and labeled for shipping within local neighborhoods and areas surrounding the facility. The packages are brought into the facility via tractor trailer trucks which will utilize only the designated loading dock positions. Once unloaded into the facility associates sort the packages by routes, place the packages onto movable racks and load the packages into the delivery vans. Sorting primarily occurs between the hours of 10:00 PM to 8:00 AM. Once loaded, the vans will exit the building via an existing drive-out door and ramp along with two new prefabricated ramps (per plan) and exit the site and onto Old 215 Frontage Road. The vans will depart the facility in "waves" beginning at approximately 10:00am and continuing every 30 minutes until approximately 1:00pm. Van deployment is intentionally staggered and scheduled outside of morning peak hours in order to minimize impact to surrounding traffic. Vans will return to the facility following the completion of their daily deliveries. Van arrival is anticipated to occur intermittently between 7:00pm and 10:00pm each evening.

In addition to the interior parking and parcel delivery operations the proposed scope of work also includes 18,715 sf of accessory office space to support the facility. Please note that the overall square footage of the office area is significantly larger than would typically be implemented in a facility of this size due to the ongoing public health crisis caused by the COVID-19 pandemic. The public areas of the office including the training rooms, restrooms, and break area have been enlarged to accommodate proper social distancing and additional programmatic spaces such as the employee screening area have been added to ensure that all current health and safety protocols are implemented daily for as long as required.

A detailed list of the proposed improvements has also been included for reference;



Transportation Engineering Services

Proposed Site Scope:

- New Parking Lot Striping to Accommodate Van Parking and Circulation
- New speed bumps/tables and directional striping per safety plan
- AMZL standard site directional and operational signage
- Surface mounted bollards in strategic locations based on van traffic flow
- Confirm ADA site compliance along accessible routes (no changes proposed)
- Ground mounted signage at entry (permit by signage vendor)
- Building mounted AMZL signage on façade (permit by signage vendor)
- Two (2) new prefabricated 12' wide "Dockzilla drive-out ramps per site plan
- New pedestrian sidewalk along West and North elevation, creating a pedestrian path of travel from the associate parking on the backside of the building to the main entrance.
- New striping required to accommodate accessible path of travel along the North elevation.
- Addition of accessory site structures (smoker's shelter & ride share shelter)

Proposed Building Shell Modifications:

- Two existing opening will be widened at the van exit locations to accommodate van traffic
- New 12'-0" wide overhead coiling doors installed at new opening locations
- Demolition of existing canopy and addition of larger entry canopy to existing façade
- Add Prime Blue accent paint to building entries per colored elevations provided
- Additional openings in roof, to accommodate new rooftop mechanical equipment per engineered Mechanical design.

Proposed TI Scope:

- Installation of new material handling conveyor system Per AMZL
- New remote restrooms, and scrubber dump
- Complete MEP design to accommodate delivery center operations. MEP scope includes but is not limited to;
 - o New warehouse lighting
 - o New rooftop mechanical units to accommodate warehouse heating and cooling
 - New exhaust fans and CO2 mitigation as required by local jurisdiction
 - o New HVLS fans per AMZL standards
 - New plumbing for remote restrooms, handwashing sinks, and scrubber dump
 - Additional electrical distribution to support warehouse operations
- Install fire extinguishers throughout per local fire authority
- Interior parking striping
- Install new high-speed fabric roll-up doors on exterior of van entry/exit locations
- New dock package at line haul locations (dock seals, dock locks, lights, fans, etc.)
 - Dock package includes associated electrical work
- New dock levelers
- New full height partition, and associated doors separating interior parking from MHE operations
- Install interior chain link fence per AMZL MHE plans
- Low Voltage/IT scope per IES Design
- Add AMZL standard restroom graphics to existing remote restrooms
- Fire sprinkler and Fire Alarm modifications as required to meet AMZL standards
- Demolition of existing speculative office area (including 3,000 sf office mezzanine) and construction of new +/- 18,715 sf office package per plan.

RECORDING REQUESTED BY PROFICIENCY 215 LLC AND WHEN RECORDED RETURN TO:

Proficiency 215 LLC 11777 San Vicente Boulevard, Suite 780 Los Angeles, CA 90049 Attention: Jeffrey Trenton Df # 2018-0342543 08/27/2018 08:00 AM Fees: \$122.00 Page 1 of 12 Recorded In Official Records County of Riverside Peter Aldana

Assessor-County Clerk-Recorder

This document was electronically submitted to the County of Riverside for recording Receipted by: ALYCIA #778

(Space Above For Recorder's Use)

COVENANT AFFECTING REAL PROPERTY

THIS COVENANT AFFECTING REAL PROPERTY ("Covenant") is made as of the 22nd day of August 2018, by Proficiency 215 LLC, a Delaware limited liability company ("Covenanter"), with reference to the following facts set forth in the recital paragraphs below:

-- RECITALS --

- A. Covenanter is the owner of the real property described more particularly in Exhibit A hereto ("Property"), subject to the jurisdiction of the March Joint Powers Authority ("JPA").
- B. The Property is situated approximately 5,440 feet northwesterly of the northwesterly terminus of runway 14-32 located at March Air Reserve Base ("MARB") and is subject to the Air Installation Compatible Use Zone Study ("AICUZ") and the March ARB/Inland Port Airport Land Use Compatibility Plan ("ALUCP"). The AICUZ and ALUCP have designated one portion of the Property as being located in Accident Potential Zone ("APZ") I and the remainder of the Property as being located in APZ II.
- C. Portions of the Property are designated in APZ I and have an occupancy limit of 25 persons per acre. Other portions of the Property are designated in APZ II and have an occupancy limit of 50 persons per acre.
- D. The Covenanter is developing the Property for an industrial warehouse building. The Covenanter has submitted to the JPA three applications for discretionary land use approvals for the development of the Property including, 1) a General Plan Amendment, assigning the JPA's General Plan designation of Industrial (I) to the Property; 2) a Change of Zone, assigning the zoning designation of Industrial (I) to the Property, consistent with the General Plan designation of Industrial (I); and 3) a Plot Plan for the proposed development of an approximately 709,083-square-foot industrial warehouse building; and environmental documentation pursuant to the California Environmental Quality Act ("CEQA"). All of the above shall be referred to collectively as the "Project."

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- E. Covenanter has agreed to comply with the Density Restrictions and a Density Cap (both terms are defined below), by limiting occupancy of the Project to (i) five hundred and eighteen (518) occupants ("Density Cap") [THE DENSITY CAP WILL DECREASE IF THE SQUARE FOOTAGE OF THE BUILDING DECREASES.]; (ii) twenty-five (25) occupants in any square area measuring 208 feet by 208 feet ("Square Area") for all Square Areas within portions of the building of the Project within APZ I; and (iii) fifty (50) occupants in any Square Area within portions of the building of the Project within APZ II. Requirements (ii) and (iii) are collectively the "Density Restrictions," and are depicted in Exhibit B, attached hereto and incorporated herein by reference. Accordingly, any building expansion is prohibited, including an increase in the building mezzanine area, without further review by JPA and MARB representatives, and consent and approval provided through an amendment to this Covenant.
- F. JPA requires that the Covenanter, and each of its successors in interest in the Property, comply with the Density Cap and Density Restrictions.
- G. JPA has indicated that it does not have the resources to undertake regular inspections of the Project to determine compliance with the Density Cap and Density Restrictions.
- H. In order to ensure that Covenanter complies with the Density Cap and Density Restrictions, upon occupancy of the Project, Covenanter agrees to fund at its sole expense quarterly inspections. These inspections shall be undertaken by a neutral independent third party to be selected by JPA ("Independent Monitor"). The JPA shall provide invoices to Covenanter concerning the Independent Monitor's costs in undertaking the activities specified in the Covenant and such costs shall be reasonable and reflect the Independent Monitor's actual costs of inspection and reporting.
- I. In order to ensure that there is compliance with the Density Cap and Density Restrictions, the Covenanter agrees to the provisions as set forth below during the term of the Covenant ("Covenant Lifespan").

- AGREEMENT -

ARTICLE 1 GENERAL PROVISIONS

- 1.1 <u>Provisions to Run with the Land</u>. Subject to the conditions of this Covenant, during the Project Lifespan, the Density Cap and Density Restrictions shall be binding upon the Covenanter and its successors and assigns in interest in the Property (collectively "Covenanter"). The Covenant, the Density Cap, and Density Restrictions shall run with the Property and each portion thereof, and shall apply to and bind the Covenanter during the Project Lifespan.
- 1.2 <u>Deemed Concurrence</u>. The Covenanter and all other persons or entities acquiring any fee interest in the Property, shall be conclusively deemed by such acquisition to have irrevocably agreed to the Density Cap and Density Restrictions for and among themselves and their heirs, successors, and assigns. Upon any such person's or entity's sale or transfer of its interest in the Property, such conveying person or entity shall forever be released and relieved of any further obligation or liability arising under this Covenant, the Density Cap, and the Density Restrictions for events arising from and after the date of such transfer.

-2-

- 1.3 <u>Incorporation into Deeds and Leases</u>. The Density Cap and Density Restrictions are hereby deemed to be incorporated by reference into each conveyance of any fee or leasehold interest in the Property or portion thereof occurring after the date the Covenant is recorded in the Official Records of Riverside County, California, whether or not referred to in the instrument affecting such conveyance of the Property, or a portion thereof.
- 1.4 <u>Parties Benefitting</u>. This Covenant inures to the benefit of MARB and JPA and their successors in interest or assigns.

ARTICLE 2 ENFORCEMENT OF COVENANT

- Right of Entry. Upon completion and occupancy of the Project, Covenanter shall 2.1 allow the Independent Monitor to enter the Project, for the sole purpose of calculating the Project occupancy and confirming that the Covenanter is complying with the Density Cap and Density Restrictions. The Independent Monitor's method for calculating the Project occupancy and confirming that the Covenanter is complying with the Density Cap and Density Restrictions shall include separate calculations of employment occupancy within APZ I and APZ II and shall be approved by JPA, at its sole discretion, prior to the commencement of inspections. Upon JPA approval of the Independent Monitor's method of calculation, JPA or the Independent Monitor shall notify the Covenanter of the Independent Monitor's methods of calculation in writing ("Methodology"), before the Independent Monitor inspects the Project, in order for Covenanter to understand the Methodology to be used by the Independent Monitor. The Methodology shall be consistent with and be based upon the calculations of the Density Restrictions. The Independent Monitor shall inspect the Project quarterly at times solely determined by the Independent Monitor within each calendar quarter. "Quarterly" shall mean each calendar quarter beginning on January 1, April 1, July 1, or October 1, as applicable, and ending on the succeeding March 31, June 30, September 30, or December 31, as applicable. Notwithstanding the foregoing, the Independent Monitor's inspections shall be done in a safe and unobtrusive manner. Within thirty (30) days of completing its quarterly inspection, the Independent Monitor shall submit written reports to MARB, JPA, and Covenanter documenting the Project occupancy. If the Independent Monitor determines that the Density Cap or Density Restrictions have been exceeded, within three (3) days of this determination, the Independent Monitor shall provide written notification to Covenanter, MARB, and JPA of this exceedance. Upon receipt of such notice, Covenanter shall eliminate the exceedance within fifteen (15) days. At the conclusion of this 15-day period, the Independent Monitor shall re-inspect the Project to determine whether the exceedance has been eliminated. If the exceedance has not been eliminated, the Independent Monitor shall provide written notification to Covenanter, MARB and JPA of this uncured exceedance no later than five (5) days thereafter and the Independent Monitor shall pursue the Administrative Remedies specified in Section 2.3 of this Covenant. If the exceedance has been eliminated, the Independent Monitor shall provide written notification no later than five (5) days thereafter to Covenanter, MARB and JPA of this elimination.
 - 2.2 <u>Remedies</u>. The remedies specified in Section 2.3 of this Covenant shall be triggered upon either (i) one (i) exceedance of the Density Cap or Density Restrictions by Covenanter which has not been corrected within the cure period set forth above in Section 2.1 in any one (1) year period of Project occupancy; or (ii) two (2) exceedances of the Density Cap or Density Restrictions by Covenanter which have been eliminated within the cure period set forth above in Section 2.1 in any one (1) year period of Project occupancy ("Actionable Default").

-3-

- 2.3 Administrative Remedies. Upon any Actionable Default, the Independent Monitor shall notify Covenanter, JPA, and MARB in writing of the Actionable Default and JPA shall commence the revocation process of the Project's certificate of occupancy pursuant to JPA Development Code § 9.02.260. If the Project's certificate of occupancy is revoked pursuant to this process, all operations and/or activities at the Project site shall immediately terminate. If the certificate of occupancy is revoked, Covenanter may reapply to JPA to re-establish the use and occupancy of the Project, subject to compliance with the Density Cap and Density Restrictions. JPA may authorize a re-establishment of the use and occupancy of the Project if Covenanter provides reasonable assurances to JPA that the Density Cap and Density Restrictions will not be exceeded in the future.
- 2.4 Mortgagee Protection. Notwithstanding any other provision of this Covenant, no breach of the Density Cap or Density Restrictions, nor the enforcement of any provisions contained in this Covenant shall affect, impair, or defeat the lien or charge of any duly recorded mortgage or deed of trust encumbering any portion of the Property, or affect, impair, or defeat the interest of the mortgagee, or its successor or assigns pursuant to such a mortgage, provided that such mortgage is made in good faith and for value. The Density Cap and Density Restrictions shall be binding upon and effective against any person whose title in the Property or any portion thereof, is derived through foreclosure, deed in lieu of foreclosure, or trustee's sale during the period of their ownership.

ARTICLE 3 MISCELLANEOUS

- 3.1 <u>No Dedication Intended.</u> Nothing herein shall be construed to be a grant or dedication, or offer to grant or dedicate, the Property or any portion thereof to MARB or JPA for any purposes whatsoever.
 - 3.2 Notices.
- (a) All notices and communications relating to this Covenant shall be in writing and shall be deemed effective when such notice or communication is personally delivered to the person:

To Covenanter:

Proficiency 215 LLC

11777 San Vicente Blvd., Suite 780

Los Angeles, CA 90049 Attention: Jeffrey Trenton

and to:

Allen Matkins Leck Gamble Mallory & Natsis LLP

1900 Main Street, 5th Floor

Irvine, CA 92614

Attention: John Condas, Esq.

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To JPA:

March Joint Powers Authority 14205 Meridian Parkway, Suite 140

Riverside, CA 92518
Attention: Danielle Wheeler

and to:

Best Best & Krieger, LLP 3390 University Avenue, 5th Floor Riverside, CA 92501

Attention: Charity B. Schiller

- (b) Concurrently with the consummation of the conveyance of an interest in the Property, or portion thereof, by the Covenanter to a successor or assign of such Covenanter, such transferee shall notify the Independent Monitor and JPA in writing of such conveyance. Such notification shall set forth the name of the transferee and its contact information for the purposes of the giving notice to such transferee under Section 3.2(a). Prior to the receipt by the Independent Monitor of any such notification, any and all written communication by the Independent Monitor under this Covenant shall be sufficient if given to the address for the Covenanter as provided in Section 3.2(a).
- 3.3 <u>Breach</u>. For all breaches of this Covenant which are not an Actionable Default, failure by Covenanter to perform an obligation which arises hereunder shall constitute a breach of this Covenant. Upon a breach of this Covenant, JPA shall commence the revocation process of the Project certificate of compliance as set forth above in Section 2.3.
- 3.4 <u>Partial Invalidity</u>. If any portion of the Covenant is determined by a judgment of a court of competent jurisdiction to be invalid for any reason, the remaining portions shall remain in full force and effect as if such portions had not been included in such a judgment.
- 3.5 <u>Successors and Assigns of Covenanter</u>. This Agreement shall be binding upon the successors in interest and assigns of Covenanter.
- 3.6 <u>Covenant Lifespan</u>. As used herein the words "Covenant Lifespan" refer to the period of time following the date of recordation of this Covenant when the Covenant is in effect. The Covenant Lifespan shall have a term of the earliest of (i) demolition of the Project; (ii) the MARB and March Inland Port Airport Authority, or subsequent civilian airport, ceasing operations as an airport; (iii) written request by MARB and March Inland Port Airport Authority to terminate the Covenant; or (iv) removal of the Project from APZ I and APZ II.
- 3.7 <u>Initiation of Enforcement Proceedings</u>. Notwithstanding any other provision of this Covenant to the contrary, no third party other than JPA may compel enforcement of any provision of this Covenant.
- 3.8 <u>Jurisdiction and Venue</u>. All legal actions arising from this Covenant shall be filed in the Superior Court of the State of California in and for the County of Riverside, California, or the United States District Court with jurisdiction in the County of Riverside, California.
- 3.9 <u>Time is of the Essence</u>. Time is of the essence in performance of the obligations set forth in this Covenant.

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IN WITNESS WHEREOF, Covenanter has executed this Covenant as of the date set forth above as evidenced by the authorized officer of Covenanter whose signature appears below.

COVENANTER

Proficiency 215 LLC,

a Delaware limited liability company

By:

effev N. Trenton

Its: President

ACKNOWLEDGMENT

A notary public or other officer completing this certificate verifies only the identity of the individual who signed the document to which this certificate is attached, and not the truthfulness, accuracy, or validity of that document.

State of California County of hold 1990(88)
on Av6 22, 2018, before me, Mark C Glode,
Notary Public, personally appeared <u>SEFFLY N-The NTOW</u> , who proved to me on the basis of satisfactory evidence to be the person(s) whose name(s) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in
his/her/their authorized capacity(ies), and that by his/her/their signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.
I certify under PENALTY OF PERJURY under the laws of the State of California that the
foregoing paragraph is true and correct.
WITNESS my hand and official seal.
Signature (Seal)

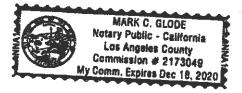


EXHIBIT A LEGAL DESCRIPTION OF THE PROPERTY

Order: QuickView_ Doc: 2018-342543 REC ALL Requested By: Printed: 9/4/2018 12:51 PM

LEGAL DESCRIPTION

THE LAND REFERRED TO HEREIN BELOW IS SITUATED IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS:

PARCEL A: (APN: 297-100-045-8)

THAT PORTION OF SECTION 15, TOWNSHIP 3 SOUTH, RANGE 4 WEST, SAN BERNARDINO MERIDIAN, IN THE COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, ACCORDING TO THE OFFICIAL PLAT THEREOF; CONVEYED TO THE CALIFORNIA SOUTHERN RAILROAD COMPANY BY DEED RECORDED JANUARY 23, 1888, IN BOOK 69, PAGE 91 OF DEEDS. IN THE OFFICE OF THE COUNTY RECORDER OF SAN BERNARDINO COUNTY, CALIFORNIA, DESCRIBED AS FOLLOWS:

COMMENCING AT THE NORTHEAST CORNER OF SAID SECTION 15, MARKED BY A 1-1/2 INCH BRASS DISC, STAMPED "RIV CO SUR-1985-SEC COR"; THENCE ALONG THE NORTH LINE OF SAID SECTION 15, NORTH 89° 52' 19" WEST, 1,461.68 FEET TO THE EASTERLY RIGHT OF WAY LINE OF THE ATCHISON, TOPEKA AND SANTA FE RAILWAY (FORMERLY CALIFORNIA SOUTHERN RAILROAD COMPANY) PER ABOVE SAID DEED AND TO THE POINT OF BEGINNING; THENCE COURSE "A", ALONG SAID EASTERLY RIGHT OF WAY LINE, SOUTH 19° 23' 12" EAST, 2678.25 FEET TO THE INTERSECTION OF THE WESTERLY PROLONGATION OF THE NORTH LINE OF THAT CERTAIN PARCEL OF LAND ACQUIRED BY THE UNITED STATES OF AMERICA BY DECREE OF TAKING, A CERTIFIED COPY OF WHICH WAS RECORDED MARCH 2, 1942, IN BOOK 532, PAGE 311, OFFICIAL RECORDS OF RIVERSIDE COUNTY, AND BY DECREE ON AMENDED DECLARATION OF TAKING, A CERTIFIED COPY OF WHICH WAS RECORDED FEBRUARY 24, 1943, IN BOOK 571, PAGE 237, OFFICIAL RECORDS OF RIVERSIDE COUNTY, WITH THE WESTERLY RIGHT OF WAY LINE OF THAT CERTAIN PARCEL OF LAND ACQUIRED BY THE STATE OF CALIFORNIA (STATE ROUTE 215), AS PARCEL NO. 35, IN DECREE OF CONDEMNATION, A CERTIFIED COPY OF WHICH WAS RECORDED MAY 18, 1943, IN BOOK 580, PAGE 327, OFFICIAL RECORDS OF RIVERSIDE COUNTY; THENCE CONTINUING ALONG SAID EASTERLY RIGHT OF WAY LINE AND SAID WESTERLY RIGHT OF WAY LINE (STATE ROUTE 215), SOUTH 19° 23' 12" EAST, 82.79 FEET TO THE BEGINNING OF A NON-TANGENT CURVE, CONCAVE NORTHEASTERLY, HAVING A RADIUS OF 875.00 FEET; THENCE NORTHWESTERLY ALONG SAID CURVE, FROM A TANGENT BEARING OF NORTH 66° 20' 39" WEST, THROUGH A CENTRAL ANGLE OF 9° 47' 09", AN ARC LENGTH OF 149.44 FEET TO THE WESTERLY RIGHT OF WAY LINE OF SAID ATCHISON, TOPEKA AND SANTA FE RAILWAY, SAID LINE BEING PARALLEL WITH AND 100.00 FEET SOUTHWESTERLY MEASURED AT RIGHT ANGLES TO THE ABOVE MENTIONED COURSE "A"; THENCE ALONG SAID WESTERLY RAILWAY RIGHT OF WAY LINE, NORTH 19° 23' 12" WEST, 2685.67 FEET TO SAID NORTH LINE OF SECTION 15; THENCE ALONG SAID NORTH LINE, SOUTH 89° 52' 19" EAST, 106.10 FEET TO THE POINT OF BEGINNING.

PARCEL B: (APN: 297-100-013-9)

PARCEL 3 (EAST): BEING THAT PORTION OF SECTION 15, TOWNSHIP 3 SOUTH, RANGE 4 WEST, SAN BERNARDINO BASE AND MERIDIAN, ALSO SHOWN AS PARCEL 3 OF RECORD OF SURVEY 000-135, ON FILE IN BOOK 110, PAGES 30 THROUGH 40, INCLUSIVE, OF RECORDS OF SURVEY, RECORDS OF RIVERSIDE COUNTY, CALIFORNIA, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHERLY TERMINUS OF THAT CERTAIN COURSE IN THE WESTERLY LINE OF THE ATCHISON, TOPEKA AND SANTA FE RAILROAD RIGHT-OF-WAY AS SHOWN ON SHEET 7 OF 11, SHEETS OF SAID RECORDS OF SURVEY, RECORDS OF RIVERSIDE COUNTY, CALIFORNIA, SAID COURSE BEARS SOUTH 19° 23' 04" EAST, 2,577.64 FEET, ALSO BEING SHOWN ON CALIFORNIA DEPARTMENT OF TRANSPORTATION MAP NO. 435571-8, ON FILE WITH THE COUNTY OF RIVERSIDE MAP NO. 205-254;

THENCE SOUTH 19° 23' 04" EAST, 2,577.64 FEET TO THE BEGINNING OF A NON-TANGENT CURVE CONCAVE NORTHEASTERLY HAVING A RADIUS OF 875.00 FEET, A RADIAL LINE TO SAID

Order: QuickView_ Doc: 2018-342543 REC ALL BEGINNING BEARS SOUTH 33° 26' 41" WEST, SAID BEGINNING ALSO BEING A POINT ON THE EASTERLY RIGHT-OF-WAY LINE OF STATE ROUTE 215, AS SHOWN ON CALIFORNIA DEPARTMENT OF TRANSPORTATION MAP NO. 435571-6, ON FILE WITH THE COUNTY OF RIVERSIDE MAP NO. 205-251;

THENCE ALONG SAID EASTERLY LINE THE FOLLOWING 9 COURSES:

- 1) NORTHWESTERLY 171.38 FEET ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 11° 13' 20";
- 2) NORTH 45° 20' 00" WEST, 391.10 FEET;
- 3) NORTH 40° 30' 27" WEST, 878.53 FEET TO THE BEGINNING OF A CURVE CONCAVE NORTHEASTERLY HAVING A RADIUS OF 4,875.00 FEET;
- 4) NORTHWESTERLY ALONG SAID CURVE 508.50 FEET THROUGH A CENTRAL ANGLE OF 05° 58' 35";
- 5) NORTH 28° 08' 58" WEST, 486.75 FEET;
- 6) NORTH 17° 49' 57" WEST, 447.33 FEET;
- 7) NORTH 30° 29' 16" EAST, 142.45 FEET;
- 8) NORTH 89° 54' 38" EAST, 415.29 FEET TO THE SOUTHERLY LINE OF ALESSANDRO BOULEVARD AS SHOWN ON CALIFORNIA DEPARTMENT OF TRANSPORTATION MAP NO. 435571-8, ON FILE WITH THE COUNTY OF RIVERSIDE MAP NO. 205-254;
- 9) THENCE ALONG SAID SOUTHERLY LINE SOUTH 89° 51' 58" EAST, 314.75 FEET TO THE POINT OF BEGINNING.

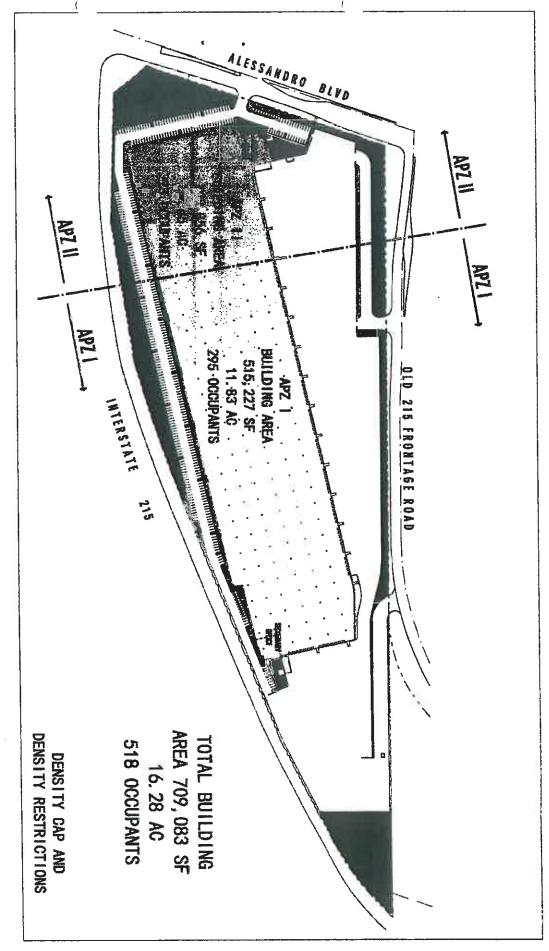
RESERVING THEREFROM ALL OIL, GAS AND OTHER MINERAL RESOURCES OF ANY KIND OR NATURE IN THE MINERAL ESTATE OF THE PROPERTY, PROVIDED, HOWEVER, THAT SUCH RESERVATION SHALL NOT INCLUDE THE RIGHT OF ACCESS TO OR ANY RIGHT TO USE ANY PORTION OF THE SURFACE OF THE PROPERTY, AS RESERVED IN DEED RECORDED DECEMBER 14, 2001, AS INSTRUMENT NO. 2001-622399, AND JUNE 25, 2002, AS INSTRUMENT NO. 2002-347891, BOTH OF OFFICIAL RECORDS.

APNs: 297-100-045-8 and 297-100-013-9

Order: QuickView_ Doc: 2018-342543 REC ALL

EXHIBIT B DENSITY CAP AND DENSITY RESTRICTIONS

Order: QuickView_ Doc: 2018-342543 REC ALL Requested By: Rrinted: 9/4/2018 12:51 PM



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NOTICE OF PUBLIC HEARING RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact ALUC Planner Paul Rull at (951) 955-6893. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The March Joint Powers Authority should be contacted on non-ALUC issues. For more information please contact March Joint Powers Authority Planner Mr. Jeffrey Smith at (951) 656-7000.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to prull@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please telephone Barbara Santos at (951) 955-5132.

PLACE OF HEARING:

Riverside County Administration Center

4080 Lemon Street, 1st Floor Board Chambers

Riverside California

DATE OF HEARING:

March 11, 2021

TIME OF HEARING:

9:30 A.M.

Pursuant to Executive Order N-25-20, this meeting will be conducted by teleconference and at the Place of Hearing, as listed above. Public access to the meeting location will be allowed, but limited to comply with the Executive Order. Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org

CASE DESCRIPTION:

ZAP1444MA20 — Jared Riemer/PR III/CHI Freeway BC, LLC (Representative: Glassman Planning Associates, MG2, and MIG. Inc.) — March Joint Powers Authority Case No. PP14-02 (Plot Plan/Determination of Substantial Conformance No. 2). The applicant proposes to revise the occupancy use and floor plan of an existing (but vacant) 709,083 square foot high-cube industrial warehouse building to allow for a Delivery Parcel Hub facility on 39.42 acres, located southerly of Alessandro Boulevard, easterly of Interstate 215, westerly of Old 215 Frontage Road, and northerly of Cactus Avenue. There is no increase to the building's footprint. The building, as amended, would provide for 258,000 square feet of high-cube warehouse area, 8,000 square feet of office area, a 1,500 square foot break room, 126 loading/queuing van spaces, and 70 indoor van parking spaces. The original shell building project (ZAP1107MA14), which proposed 694,083 square feet of high-cube logistics warehouse, 12,000 square feet of first floor office area, and 3,000 square feet of second floor office mezzanine, was found consistent by ALUC in 2015. A subsequent revision to the high-cube warehouse building (ZAP1394MA19) was proposed to provide an additional 10,000 square feet of office area (by reducing warehouse area by the same square footage) was also found consistent by ALUC in 2020 (Airport Compatibility Zones B1-APZ-I and B1-APZ-II of the March Air Reserve Base/Inland Port Airport Influence Area).



RIVERSIDE COULTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

Jeach New Cas ALUC CASE NUMBER:	0.4	2020	powe William WV		
APPLICANT / REPRESENTA	ATTVE / PROPERTY OWNER CONTACT INFORMATION				
Applicant	Glassman Planning Associates / Contact: David Glassman	Phone Number	310-781-8250		
Mailing Address	1111 Sartori Ave.	Email david@	gpan.com		
	Torrance, CA 90501				
Representative	MG2 / Contact: Mike Miranda	Phone Number	206-962-6589		
Mailing Address	1101 2nd Ave #100	_{Email} mike.n	niranda@mg2.com		
_	Seattle, WA 98101				
Property Owner	PR III - CHI Freeway BC / Contact: Jared Riemer	Phone Number	213-631-2336		
Mailing Address	527 W 7th St, Ste #308	_{Email} jrieme	r@chindustrial.com		
-	Los Angeles, CA 90014				
LOCAL JURISDICTION AG	ENCY				
Local Agency Name	March Joint Powers Authority	Phone Number	951-807-7283		
Staff Contact	Jeff Smith	Email .			
Mailing Address	14205 Meridian Parkway, Suite 140		ndition Use Permit		
Local Agency Project No	PLOT PURP 14-02. SUBTRATING CHESTING 2	Zoning Ordina	Specific Plan Amendment nce Amendment rcel Map / Tentative Tract ew/Plot Plan		
PROJECT LOCATION			and a		
Attach an accurately scaled i	map showing the relationship of the project site to the airport boundary and runways				
Street Address	2677 E Alessandro Blvd,				
	Riverside, CA 92553				
Assessor's Parcel No.	297-100-045, -087	_ Gross Parcel Size			
Subdivision Name	N 1/2 OF SEC 15 T.3S., R.4W	Nearest Airport distance from A			
Lot Number	87 / 45	_ port	Riverside Municipal Airport (10.5 Miles		
PROJECT DESCRIPTION if applicable, attach a detail tional project description da	led site plan showing ground elevations, the location of structures, open spaces and water bo Ita as needed	dies, and the heights (of structures and trees; include addi		
Existing Land Use	The site currently consists of a recently completed specu	lative industria	al building, and		
(describe)	associated site and public improvements as previously ar				
Į.	and Plot Plan 14-02. The building includes a speculative office area, including a 3,000 sf				
	mezzanine, and two remote warehouse restrooms, but th	e building is c	currently vacant.		

Riverside County Airport Land Use Commission, County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, CA 92501, Phone: 951-955-5132 Fax: 951-955-5177 Website: www.realuc.org

Proposed Land Use	The proposed scope of work is a first generation tenant in	mprovement. The proposed use is a Parcel Delivery
(describe)	Station which is an allowed use per Chapter 9.02 / Table 9	
	The tenant space will occupy the entire 706.081 sf of b	uilding area including 252 interior parking stalls,
	215,000 sf of parcel delivery operations, & 18,715 sf of office	which will replace the existing speculative office area.
For Residential Uses For Other Land Uses	Number of Parcels or Units on Site (exclude secondary units) Hours of Operation 24 Hours	N/A
(See Appendix C)	Number of People on Site 172 Maximum Number 298	er Tenant Operations + 126 Interior Van
	Loading Spaces = 298	Total Max Occupants.
Height Data	Site Elevation (above mean sea level)	FFE = -0.67' ft.
	Height of buildings or structures (from the ground)	44'-6" (Existing) ft.
Flight Hazards	Does the project involve any characteristics which could create elec- confusing lights, glare, smoke, or other electrical or visual hazards to	
	If yes, describe	

- A. NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME: Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:
 - 1..... Completed ALUC Application Form.
 - 1. ALUC fee payment Total Fee = \$4,021 (See Fee Schedule for Additional Information)
 - 1..... Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
 - 1..... Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
 - 1..... CD with digital files of the plans (pdf)
 - 1..... Vicinity Map (8.5x11)
 - 1.... Detailed project description
 - 1..... Local jurisdiction project transmittal Previously Provided By Jeff Smith March JPA
 - 3. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
 - Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. (Only required if the project is scheduled for a public hearing Commission meeting)

COUNTY OF RIVERSIDE AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM: 3.5 2.2

HEARING DATE: May 13, 2021 (Continued from April 8, 2021)

CASE NUMBER: ZAP1456MA21 – Penske Truck Leasing Co. L.P/CD Moreno

Holding LLC)

APPROVING JURISDICTION: City of Moreno Valley

JURISDICTION CASE NO: PEN21-0025 (Plot Plan)

LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use

Compatibility Plan

Airport Influence Area: March Air Reserve Base

Land Use Policy: Zones B1-APZ-I and B1-APZ-II

Noise Levels: 65-70 CNEL

MAJOR ISSUES: The project was continued from the April 8, 2021, meeting, pending a new analysis of the revised site design introduced by the applicant on March 25, 2021 (from one building to two buildings). This new site layout has been reviewed and analyzed in this staff report and was re-advertised to the public.

Also, at the April 8, 2021, meeting, the Air Force provided their comment letter dated April 6, 2021. The letter identified that the use of the Covenant was encouraged by the Air Force, with the ultimate solution being the completion of the March Air Reserve Base Compatible Use Study and the subsequent update to the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, which addresses the intensity inconsistency between the 2014 March ALUCP and the Department of Defense Instruction No. 4165.57.

The applicant is proposing a revised project and site design layout from what was previously reviewed and found consistent by the Commission in ALUC project's ZAP1038MA07 and ZAP1333MA18. A breakdown of the proposed Penske Truck rental and repair facility indicates that the proposed project's average and single acre intensities are consistent with the Compatibility Zone B1-APZ-I criteria.

However, the resulting project intensity exceeds the Air Force's interpretation of Air Force Instruction 32-7063 dated December 18, 2015, which addresses Air Force policies on Land Use Compatibility in accordance with Department of Defense Instruction (DoDI) No. 4165.57, for

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APZ-I and APZ-II. The Air Force understands the DoDI as limiting intensity to a maximum of 25 people in any given acre in APZ-I, and a maximum of 50 people in any given acre in APZ-II.

In order to address this issue, the applicant has agreed to a condition requiring the recording of a Covenant on the title of the property, restricting actual occupancy of the building to a maximum of 25 people in any given acre in APZ-I. Operation in compliance with this covenant will be necessary to satisfy Air Force and City of Moreno Valley concerns regarding project intensity.

RECOMMENDATION: Staff recommends that the Plot Plan be found <u>CONSISTENT</u>, subject to the conditions included herein. <u>CONTINUE</u> the matter to the May 13 June 10, 2021, meeting, pending completion of the Air Force review of the project.

PROJECT DESCRIPTION: The applicant proposes to construct a 21,155 square foot Penske Truck facility including a 19,200 square foot building, a 1,792 square foot building and a 1,936 square foot truck fueling island on 9.10 acres.

On October 11, 2007, the Commission found the original 16-building (totaling 253,740 square feet) business park on 19.56 acres (ZAP1038MA07), consistent with the 1984 Riverside County Airport Land Use Plan, as applied to March Air Reserve Base/Inland Port Airport.

On December 18, 2018, the Commission found a proposal to construct 10 industrial warehouse buildings (totaling 122,979 square feet) on 9.37 acres, and a proposal to develop a 15,280 square foot Penske Truck building on 9.86 acres, consistent with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan.

PROJECT LOCATION: The site is located southerly of Alessandro Boulevard, westerly of Day Street, and easterly of Old Highway 215 Frontage Road, within the City of Moreno Valley, approximately 6,350 feet northwesterly of the northerly end of Runway 14-32 at March Air Reserve Base.

BACKGROUND:

Non-Residential Average Land Use Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zones B1-APZ-I (8.56 acres) and B1-APZ-II (0.36 acres). Zone B1-APZ-I limits average intensity to 25 people per acre, and APZ-II limits average intensity to 50 people per acre. The majority of the site's development footprint (**two** buildings, truck fuel island) is located within Zone B1-APZ-I, whereas truck parking and landscaping are located within Zone B1-APZ-II.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan and the Additional Compatibility Policies included in the March ALUCP, the following rates were used to calculate the occupancy for the proposed project:

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- Assembly (conference room, breakroom, lobby) = 1 person per 15 square feet,
- Office 1 person per 200 square feet,
- Storage 1 person per 300 square feet,
- Warehouse 1 person per 500 square feet, and
- ▶ Service Bay/Wash Bay/Truck Fueling spaces 1.5 persons per bay door/fueling spaces.

The proposed project two buildings includes a total of 1,048 square feet of office area, 1,797 square feet of warehouse area, 2,830 square feet of storage area, 326 square feet of break room area, 420 square feet of lobby area, 192 square feet of conference room area, 5 service repair bay doors, 1 wash bay door, and two truck fueling spaces, 433 square feet of lobby area, 1,995 square feet of office area, 1,644 square feet of warehouse area, 1,208 square feet of storage area, 348 square feet of break room area, 180 square feet of conference room area accommodating an occupancy of 93 94 people, resulting in an average intensity of 10 11 people per acre, which is consistent with the Zone B1-APZ-I average criterion of 25 people per acre.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per standard vehicle and 1.0 persons per trailer truck in the absence of more precise data). Based on the number of parking spaces provided (46 44 standard vehicles, 141 148 truck spaces), accommodating a total occupancy of 210 214 people, resulting in an average intensity of 23 25 people per acre, which is consistent with the Zone B1-APZ-I average criterion of 25 people per acre.

Non-Residential Single-Acre Land Use Intensity: Compatibility Zones B1-APZ-I and APZ-II limit maximum single-acre intensity to 100 people. There are no risk-reduction design bonuses available, as March Air Reserve Base/Inland Port Airport is primarily utilized by large aircraft weighing more than 12,500 pounds.

Based on the **revised** site plan provided and the occupancies as previously noted, the maximum single-acre intensity in B1-APZ-I is located around building 1 and includes 639 square feet of office area, 1,797 square feet of warehouse area, 2,564 square feet of storage area, 326 square feet of break room area, 1 wash bay, 5 service repair bay doors, and two truck fueling spaces, 433 square feet of lobby area, 1,995 square feet of office area, 1,644 square feet of warehouse area, 1,208 square feet of storage area, 348 square feet of break room area, 180 square feet of conference room area accommodating a total occupancy of 92 50 people, which is consistent with the Compatibility Zone B1-APZ-I single acre intensity criterion of 100.

Although the abovementioned single acre intensity in B1-APZ-I is consistent with the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, it is inconsistent with the Air Force Department of Defense Instruction No. 4165.57 with regards to intensity, which is limited to a maximum of 25 people in any given acre in APZ-I, and 50 people in APZ-II. A more detailed analysis is provided below in the March Air Reserve Base section of the staff report.

March Air Reserve Base/United States Air Force Input: Given that the project site is located in Zones B1-APZ-I and B1-APZ-II of the primary runway at March Air Reserve Base, the March Air

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Reserve Base staff was notified of the project and sent a package of plans for their review. As of the time this staff report was prepared, we were still awaiting comments from the Air Force regarding this project.

The 2018 Airport Installation Compatible Use Zones (AICUZ) study identifies the project site as located within Accident Potential Zone II (APZ-II). Appendix A of the AICUZ provides Land Use Compatibility Tables for the APZs, which cite "warehousing" and "repair services" as permitted uses in APZ-I and APZ-II (and prohibited use in the Clear Zone [CZ]).

However, March Air Reserve Base officials maintain that the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan is not consistent with current Air Force guidance found in Air Force Instruction 32-7063 dated December 18, 2015, which addresses Air Force policies on Land Use Compatibility in accordance with Department of Defense Instruction (DoDI) No. 4165.57. These inconsistencies include conflicts with regard to lot coverage, intensity, and permitted use definitions.

The proposed project complies with the restrictions on permitted uses and lot coverage, but not with the intensity limits. The Air Force understands the DoDI criteria as limiting intensity to a maximum of 25 people in any given acre in APZ-I and to a maximum of 50 people in any given acre in APZ-II. As noted above, the project would be expected to result in a single acre occupancy of 100 50 people in APZ-II.

The projected occupancy intensities would be inconsistent with the Air Force intensity understanding.

One method of bringing the project into consistency with both the Air Force Instruction (AFI) is for the applicant to agree to a condition including a Covenant, recorded on the title of the property, restricting the actual occupancy of the building to the limits of the AFI.

The applicant has agreed to this condition, which limits actual occupancy of the building to 25 persons in any given acre within APZ-I and to 50 people persons in any given acre within APZ-II. Specifically, the Covenant states:

E. Covenanter has agreed to comply with the Density Restrictions and a Density Cap (both terms are defined below), by limiting occupancy of the Project to (i) twelve (12) occupants ("Density Cap") [THE DENSITY CAP WILL DECREASE IF THE SQUARE FOOTAGE OF THE BUILDING DECREASES.]; (ii) twenty-five (25) occupants in any square area measuring 208 feet by 208 feet ("Square Area") for all Square Areas within portions of the building of the Project within APZ I, and (iii) fifty (50) occupants in any Square Area within portions of the building of the Project within APZ II. Requirement (ii) and (iii) are collectively the "Density Restrictions", and are depicted in Exhibit B, attached hereto and incorporated herein by reference. Accordingly, any building expansion is prohibited, including an increase in building area, without further review by the City and MARB representatives, and consent and approval provided through an amendment to this covenant.

The Air Force provided a comment letter dated April 6, 2021, which was submitted at the April 6, 2021, ALUC meeting. The letter identified that the use of the Covenant was encouraged by the Air Force, with the ultimate solution being the completion of the March Air Reserve Compatible Use Study and the subsequent update to the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, which addresses the intensity inconsistency between the 2014 March ALUCP and the Department of Defense Instruction No. 4165.57.

Prohibited and Discouraged Uses: The applicant does not propose any prohibited or discouraged uses in Compatibility Zones B1-APZ-I and B1-APZ-II. Industrial warehouse buildings and repair services are compatible within Accident Potential Zones I and II pursuant to the 2018 Air Installation Compatible Use Zone (AICUZ) study disseminated by the United States Air Force. Use as an industrial warehouse is also compatible pursuant to Department of Defense Instruction (DoDI) No. 4165.57, but the intensity levels of this project in the absence of the Covenant would exceed DoDI allowances, as understood by the Air Force.

Noise: The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being within the 65-70 CNEL range. While the proposed warehouse/service repair facility is not a noise-sensitive use and would not require special measures to mitigate aircraft-generated noise, such measures may be required to achieve reduced interior noise levels of 45 dBA CNEL in office areas as required pursuant to the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan.

Part 77: The elevation of Runway 14-32 at its northerly terminus is approximately 1,535 feet above mean sea level (1,535 feet AMSL). At a distance of approximately 6,350 feet from the runway, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,598 feet AMSL. The project's site elevation is 1,540 feet AMSL. With a maximum building height of 25 feet, the top point elevation would be 1,565 feet AMSL. Therefore, review of this building by the FAA Obstruction Evaluation Service (FAA OES) is not required.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically. However, development within Compatibility Zone B1-APZ-I and APZ-II is limited to a maximum lot coverage of 50%. The maximum lot coverage proposed is 6%, which is consistent with the maximum lot coverage criterion.

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated (Section 2.3.2 of FAA Advisory Circular 5200-33C). The project is located 7,835 feet from the runway, and therefore would be subject to the above requirement.

Although the nearest portion of the proposed project is located within 10,000 feet of the runway (approximately 7,835 feet), the project utilizes underground detention systems which will not contain surface water or attract wildlife and, therefore, would not constitute a hazard to flight.

CONDITIONS:

- 1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
- 2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight or circling climb following takeoff or toward an aircraft engaged in a straight or circling final approach toward a landing at an airport, other than a DoD or FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight or circling climb following takeoff or towards an aircraft engaged in a straight or circling final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, congregate care facilities, hotels/motels, places of assembly (including, but not limited to places of worship and theaters), buildings with more than 1 aboveground habitable floors, hazardous materials and critical community infrastructure facilities.
 - (f) Highly noise-sensitive outdoor non-residential uses. Examples of noise-sensitive outdoor nonresidential uses that are prohibited include, but are not limited to, major spectator-oriented sports stadiums, amphitheaters, concert halls, and drive-ins.
 - (g) Other hazards to flight.

- 3. Prior to issuance of any building permits, the landowner shall convey and have recorded an avigation easement to the March Inland Port Airport Authority or its successor in interested, or provide evidence that such easement has previously conveyed. The Airport Authority may waive this requirement in the event that the Authority determines that pre-existing avigation easements dedicated to the United States of America are sufficient to address its needs. Contact the March Joint Powers Authority at (951) 656-7000 for additional information.
- 4. The attached notice shall be given to all prospective purchasers and/or tenants of the property. While not required, the applicant and its successors-in-interest are encouraged to provide a copy of said notice to employees who would regularly be working at this location.
- 5. The project has been conditioned to utilize underground detention systems, which shall not contain surface water or attract wildlife. Any other proposed basin would require review and approval by the ALUC. Any new detention basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the detention basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

- 6. Office space must have sound attenuation features sufficient to reduce interior noise levels from exterior aviation-related sources to no more than CNEL 45 dB. March Joint Powers Authority shall require an acoustical study to ensure compliance with this requirement.
- This project has been evaluated for 1,048 square feet of office area, 1,797 square feet of warehouse area, 2,830 square feet of storage area, 326 square feet of break room area, 420 square feet of lobby area, 192 square feet of conference room area, 433 square feet of lobby area, 1,995 square feet of office area, 1,644 square feet of warehouse area, 1,208 square feet of storage area, 348 square feet of break room area, 180 square feet of conference room area 5 service repair bay doors, 1 wash bay door, and two truck fueling spaces. Any

increase in building area, change in use to any higher intensity use, change in building location, or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.

- 8. All buildings shall be designed with zoned fire sprinkler systems.
- 9. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
- 10. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission and March Air Reserve Base.
- 11. In order to ensure proper functioning of the project drain system to avoid potential hazards to March Air Reserve Base flights, an additional Best Management Practice (BMP) shall be added to the project Water Quality Management Plan (WQMP). The applicant shall enter into a covenant and agreement with the City of Riverside similar to the Water Quality Management Plan and Urban Runoff BMP Transfer, Access and Maintenance Agreement between March Joint Powers Authority and Sun Life Assurance Company of Canada (Document No. 2014-0030862), which shall be recorded prior to issuance of a certificate of occupancy. A copy of the recorded agreement and BMP shall be provided to the Riverside County Airport Land Use Commission. The BMP shall include the following program:
 - a. The property owner (Alessandro Property Investments LP or its successor(s)-in-interest, hereinafter "Owner") or its designated representative shall monitor the conditions of the detention basins and promptly inspect such basins following the completion of each "significant" rain event and the 48-hour period thereafter.
 - b. If any standing water remains in a basin that is not beneath a rock, gravel, or other layer following the completion of the "significant" rain event and the 48 hour period thereafter, Owner or its designated representative shall arrange to have such standing water either removed or covered within the next two business days following the conclusion of the 48 hour period.
 - c. In the event that the standing water situation recurs on a regular basis following the 48-hour detention period, the detention basin may no longer be draining as originally designed to prevent standing water from rising above a rock, gravel or other layer (for example, due to a rise in groundwater levels or other circumstance beyond Owner's ability to control). In that situation, Owner or its designated representative shall promptly engage a licensed civil engineer to prepare a design plan to assure that such

condition does not persist for more than 48 hours following the conclusion of a "significant" rain event. The required engineering design solution shall be implemented promptly, but no later than 180 days following its approval by all applicable authorities, providing that, until such time as the engineered design solution is implemented, Owner or its designated representative will maintain water levels below the rock, gravel, or other layer.

12. The applicant has agreed to accept a Covenant which will be recorded on the title of the property restricting the actual occupancy of the buildings to the limits of the Air Force Instruction. The project shall be incompliance with the recorded and executed Covenant, which limits building occupancy to a maximum of 25 people in any given acre in APZ-I, and a maximum of 50 people in any given acre in APZ-II. The Covenant shall include the following language:

"Covenanter has agreed to comply with the Density Restrictions and a Density Cap (both terms are defined below), by limiting occupancy of the Project to (i) twelve (12) occupants ("Density Cap") [THE DENSITY CAP WILL DECREASE IF THE SQUARE FOOTAGE OF THE BUILDING DECREASES.]; (ii) twenty-five (25) occupants in any square area measuring 208 feet by 208 feet ("Square Area") for all Square Areas within portions of the building of the Project within APZ I, and (iii) fifty (50) occupants in any Square Area within portions of the building of the Project within APZ II. Requirement (ii) and (iii) are collectively the "Density Restrictions", and are depicted in Exhibit B, attached hereto and incorporated herein by reference. Accordingly, any building expansion is prohibited, including an increase in building area, without further review by the City and MARB representatives, and consent and approval provided through an amendment to this covenant."

Compliance shall be verified by City or third-party inspections and reports on a schedule agreed upon by the applicant/project operator, the City, and MARB representatives.

Y:\AIRPORT CASE FILES\March\ZAP1456MA21\ZAP1456MA21MAYsr.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b)

NOTICE

THERE IS AN AIRPORT NEARBY.

THIS STORM WATER BASIN IS DESIGNED TO HOLD

STORM WATER FOR ONLY 48 HOURS AND

NOT TO ATTRACT BIRDS

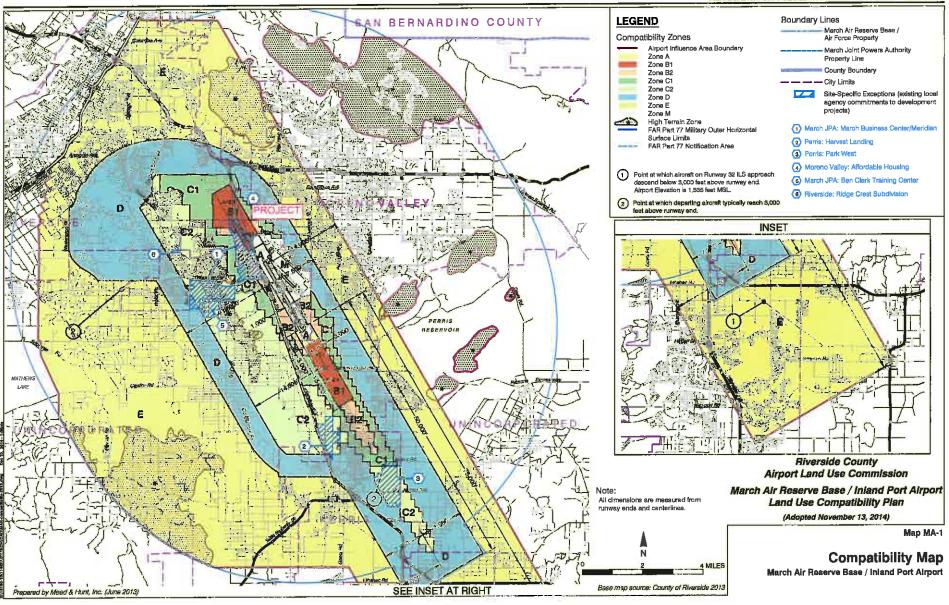
PROPER MAINTENANCE IS NECESSARY TO AVOID BIRD STRIKES

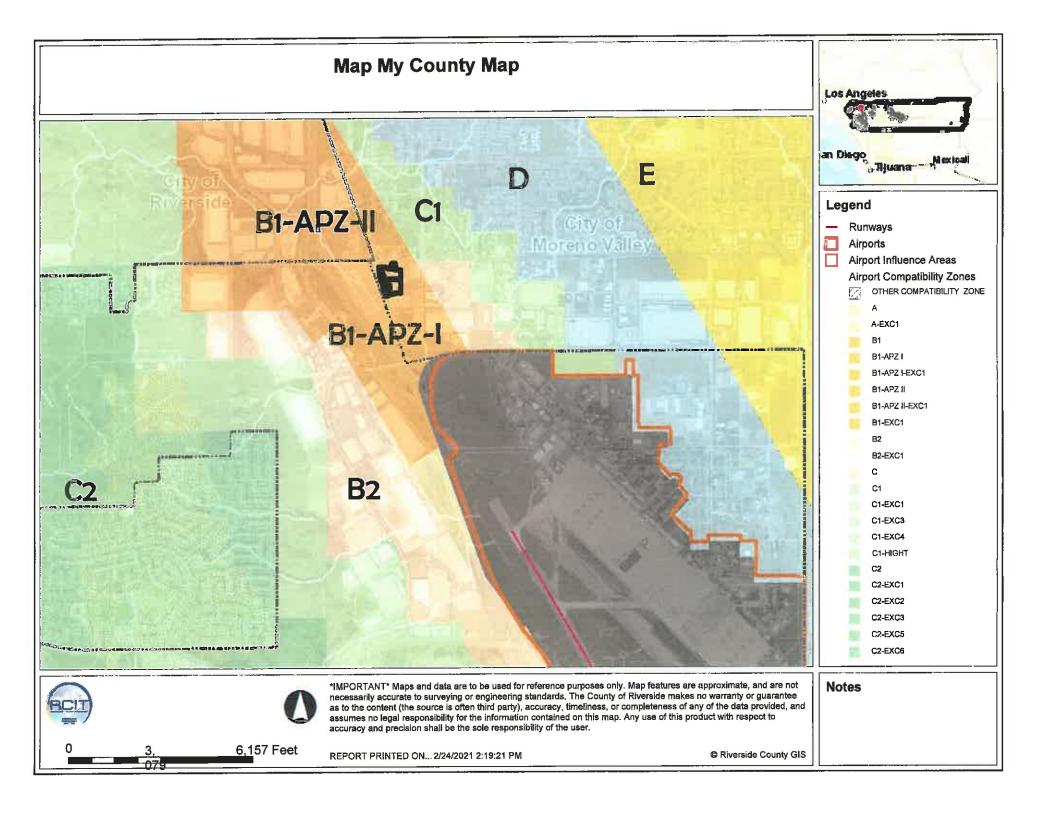


IF	THIS	BASIN	IS	OVERGROWN	Pl	.E/	ASE	CONT	TACT	Γ:
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Name:	 Phone:	







Map My County Map Los Angeles ் Jijuana - Mexicali Legend B1-APZ-II Runways Airports Airport Influence Areas Airport Compatibility Zones OTHER COMPATIBILITY ZONE Α A-EXC1 В1 B1-APZ I City of Moreno Valley B2 B1-APZ I-EXC1 B1-APZ II 81-APZ II-EXC1 B1-EXC1 В2 B2-EXC1 С Ç1 C1-EXC1 B1-APZ-C1-EXC3 C1-EXC4 C1-HIGHT CZ C2-EXC1 C2-EXC2 C2-EXC3 C2-EXC5 C2-EXC6 *IMPORTANT* Maps and data are to be used for reference purposes only. Map features are approximate, and are not **Notes** necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user. 770 1,539 Feet C Riverside County GIS REPORT PRINTED ON... 2/24/2021 2:17:47 PM

Map My County Map





Legend

Blueline Streams
City Areas
World Street Map





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Notes

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Legend

Blueline Streams City Areas World Street Map





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Notes

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DEPARTMENT OF THE AIR FORCE AIR FORCE RESERVE COMMAND

April 6, 2021

Colonel Rodney E. McCraine Commander 452d Mission Support Group 1261 Graeber Street, Bldg. 2313, Suite 135 March ARB, CA 92518

Mr. Paul Rull Director Riverside County Airport Land Use Commission 4080 Lemon Street, 14th Floor Riverside, CA 92501

Dear Mr. Paul Rull,

This is in response to three applications, listed below, being considered by the Riverside County Airport Land Use Commission (ALUC) and addresses the application of Department of Defense Instruction (DODI) 4165.57 and Office Density Restriction Covenants for Riverside County Airport Land Use Commission (ALUC) on these cases –

- ZAP1444MA20 (Amazon Parcel Delivery Hub at Freeway Business Center, March JPA),
- ZAP1452MA21 (OZI Warehouse, City of Riverside),
- ZAP1456MA21 (Penske Truck Facility, City of Moreno Valley)

With respect to the 3 projects, and for the any future projects and applications, March ARB encourages the ALUC to follow the DODI 4165.57 as a minimum guide to determining office density restrictions and appropriate covenants to be used in ensuring that the density restrictions are met. The Instruction encourages local governments to address labor intensity, which addresses the allowable number of persons within a building and even in portions of the buildings. The most recent F-16 aircraft accident in March 2019 near March ARB is an example of the risk to property and life that exists when developments encroach upon an Air Base. Luckily, the F-16 crashed in a portion of the building with very low labor intensity and no one was injured. However, the accident could have easily happened only a few feet to the west where the higher labor intensity of the office space in that building could have resulted in serious bodily injury or death.

The ALUC should always consider the risk of an aircraft accident when making a decision. The use of Accident Potential Zones (APZ) described in the DODI 4165.57 address the general risk, but the DODI is only minimal guidance for use by local authorities to reduce the risk to the general public. There are two equally important aspects to the risk: the potential for aircraft to have an accident, and the potential for the public to be impacted by an accident. The Air Force (AF) is ready to work with ALUC to address March ARB risks and approaches to reduce the potential for accidental injury or death to the public both in and outside of allowable developments within the APZs. It is the Air Forces desire that the ALUC would side with conservative limits on building and development occupations.

The ALUC and local governments can do much to reduce potential accidents by having strong codes to reduce the cumulative development impact on air-pollution, electronic interference with aircraft, height of structures, and potential glare to pilots. Likewise, ALUC and local governments can greatly reduce the potential risk to the public by strong codes to reduce occupation density of land uses within the APZs, not only within certain land uses, but also labor intensity within building uses in the APZs.

ALUC's use of deed covenants to restrict building occupation in developments, which are not located in a city with adequate land use controls, is encouraged by the AF. The specific wording must ensure that the entity having jurisdiction be able to enforce the covenant. The AF is not able to inspect or enforce any deed covenants. This is a local government responsibility and authority. Covenants are not easily enforced and strict land use controls by the City and County are the real solution to not only reduce the risk but to enforce the reduced risk in the future.

Finally the AF will work with the ALUC in its Compatibility Use Study to develop a Compatibility Use Plan to develop further guidance for local governments to address and enforce; occupation density in APZs, labor intensity within buildings and other factors, to include the cumulative impacts of multiple building solar glare, air emissions, bird and wildlife hazards, etc. We look forward to working with the ALUC and its member governments in this regard.

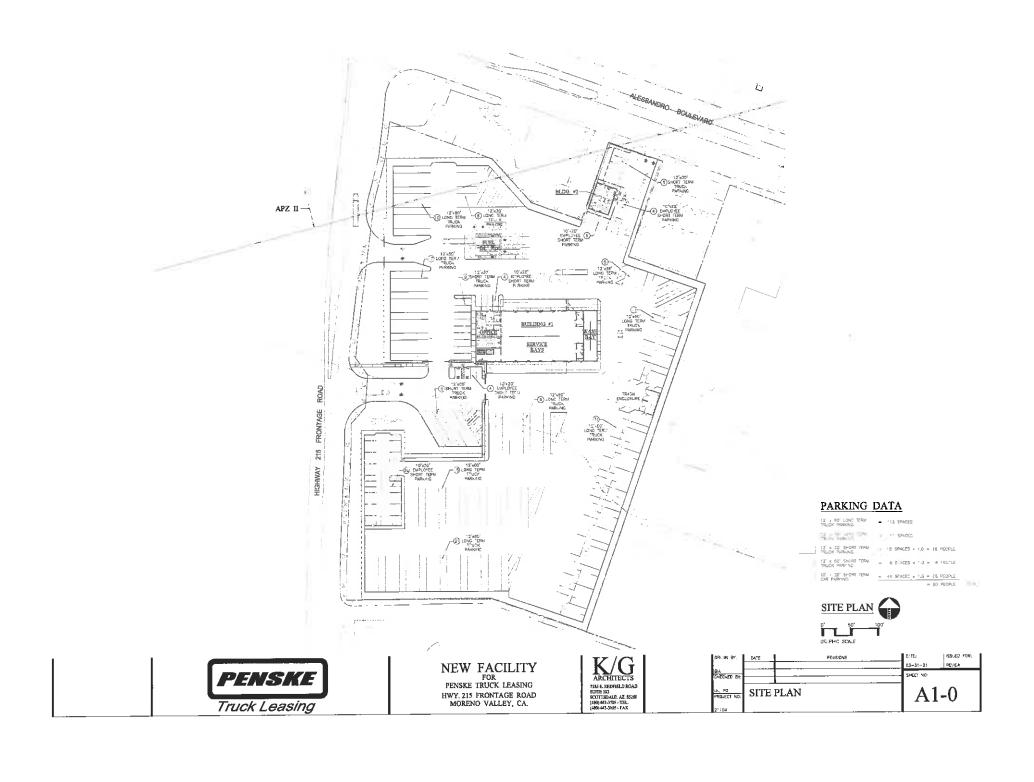
Should you have any questions or would like to discuss this further, please feel free to contact me directly at (951) 655-3700.

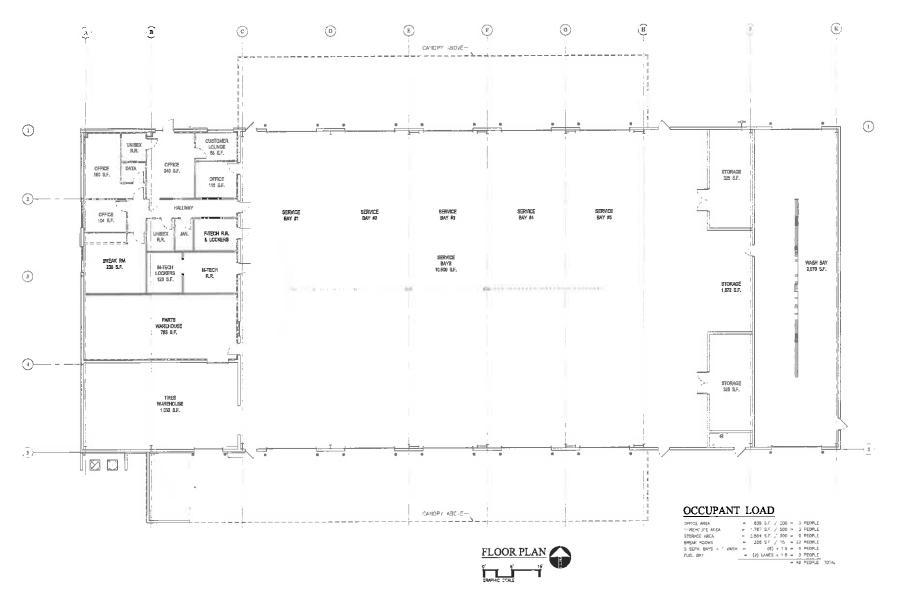
Sincerely

MCCRAINE.RO Digitally signed by DNEY.ERIC.10 C.1067944492 67944492

MCCRAINE.RODNEY.ERI Date: 2021.04.05 15:55:27 -07'00'

RODNEY E. MCCRAINE, Colonel, USAF Commander, 452d Mission Support Group







NEW FACILITY
FOR
PENSKE TRUCK LEASING

FOR
PENSKE TRUCK LEASING
HWY. 215 FRONTAGE ROAD
MORENO VALLEY, CA.



DRE./N BY.	DATE	#EVISIONS	DATE: 03-31-21	SSIED FOR:
BIA CHECKED BY: LN, PG PROJECT NO:	BUIL	DING #1	SMEET NO:	2-0
21104	OCCL	JPANCY PLAN	1 11	





 OCCUPANT LOAD

 Recka, Jobs
 = 420 SF, / 18 = 28 PEOPLE

 OFFICE AREA
 = 408 SF, / 300 = 2 PEOPLE

 STORAGE AREA
 = 286 SF, / 300 = 1 PEOPLE

 CONFERENCE PROM
 = 187 SF, / 18 = 13 PEOPLE

 44 PEOPLE

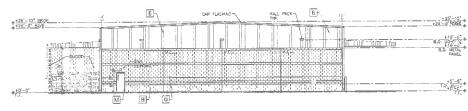


NEW FACILITY FOR PENSKE TRUCK LEASING HWY. 215 FRONTAGE ROAD MORENO VALLEY, CA.

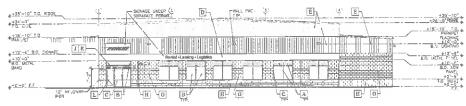
K/G	
7585 E. REDFIELD ROAD	
SUITE 102	
SCOTTSDALE, AZ. \$5260	
(480) 443-3705 - TEL.	
(480) 443-3FD5 - FAX	

DATE:	ISSUED FOR:
	<u> </u>
<i>F</i>	\2-1
	0.7E: 0.3-3:-21 SHEET NO

MARK	MATERIAL	LOCATION	MANUFACTURERS	DESCRIPTION
A	GL-2 BECNOPEL CLASS	ABO'S BET WHERE NOTED AT EXTERIOR WINDOWS	-	1" INSULATED SPANDREL GLAZING, COLOR; BLACK OPACIAE — TEMPERED AS REQUIRED
В	GU-1 VIEWN CLASS	TYPICAL AT EXTERIOR DODRS AND WHOOMS AT INVIED	-	1" INCLUATED GLAZING, COLOR: LIGHT GRAYINT WITH LC'Y 'E' COATING ON \$3 SUPFACE - "EMPERED A3 REQUIRED
C	SF-1 STDREFRONT FRAMING	STOREFRONT DOOR & WINDOW LOGATIONS	KAYNEER OR EQUAL	CLEAR ANGDIZED ALUMINUM STOREFRONT FRAUNG
D	MP-1 METAL PANEL (YELLOW)	REHTAL. JER NOE & OFFICE ARCHONS - BUILDING BANDING	BY PENINE VENDOR	A.C.H. METAL EAND: ACM COLOR: YELLOW
E	MP-2 VETAL PANEL (BROWN)	ANP METAL FACADE PANELS, DOWNEPOUTS & TRIM	METAL BUILDING LENDOR	ATP METAL BUILDING PANELS, DOWNSPOUTS AND TRA COLOR MEDIUM BRONZE
F	D-3 INSULATED C'ERHEAD DOOR	SEPVICE AND WASH BATS	DVERHEAD DOOR: SEPIES 592	EXTERIOR PAINT COLOR: ARCHITECTURAL BROWN
G	UU-2 NU ONRY UNIT	BULLD-NG FACADE ASO € 2'-B"	B" . B" 116" STANDARD GRAY S	+2'-8" UPPER MALL SPLIT FACE NASONRY YITH ELASTOMERIC PAINT COLOR EAR-11
H	BU-1 K. ONRY UNIT	BUYLDING BASE TO 2"-8"	ST 4 8" X16" STANDERD G - 47	UP TO 2"-8" A.F.F., SPUT FACE MASONRY WITH ELASTONERIC PAINT COLOR EXP-10
Ι	BC-1 BOLLAND DOVER	SENINCE BUILDING ENTERIOR AT TOORS. AND UTILITIES	BY IDEAL SHELD	STEEL PIPE BOLLARCT WITH ELECKE COVERS
J	PC-2 ALUMNUM BAND	ABOVE STOREFRONT ENTRINCES	KAUNIEER OR EQUAL	9 DWDER CONTED ALUMINUM BAND COLOR: AEGEAN BLUE (UPPER BAND)
K	PC-1 ALUMNUM BAND	ABOVE STOREFRONT ENTRANCES	KEWNEER OR EQUAL	PONDER CONTED ALUMINUM BAND CONOR: ULTRUMARINE BLUE (UPPER BAND)
L	D-1 ALUMINUM STOREFRONT	ENTRY DOOR; RENTAL, OFFICE & SERVICE ENTRANCE	KAN VEER OR EQUAL	FULL VIE'011 080P
M	9-2 HOLLOW WETAL DOOR	RENTAL STORIGE / CHOP ENTRY /	STEEL MAFT E-SER-ES OP DECO MEDILLION SERIES	PRE-FINISHED ARCH BROWN COLOR EXP-3



EAST ELEVATION



WEST ELEVATION SOUR 3/32 = 1 -0



3 SOUTH ELEVATION



NORTH ELEVATION



NEW FACILITY

FOR
PENSKE TRUCK LEASING
HWY, 215 FRONTAGE ROAD
MORENO VALLEY, CA.

ARCHITECTS
1383 E REDFIELD ROAD
5UTIS 102
SCOTTSDALE, AZ. 83280
(480) 443-3805 - FAX

ORANN BY DATE	PEVISION*	DATE. D2-19-21	ISSUED FOR:
BIA DHECKED BY:		SHEET YO:	
PROJECT NO: ELEVA	TIONS	A	7-0



Penske Truck Leasing 1711 W. Greentree Dr. Suite 117 Tempe, AZ 85284

Tel: 602-253-0041 Fax: 480-7754350

www.pensketruckleasing.com

March 25, 2021

Riverside County Airport Land Use Commission 40480 Lemon Street., 14th. Floor Riverside, CA 92501 Attn: Paul Rull, ALUC Principal Planner

RE: ZAP1456MA21 ALUC Review

Dear Mr Rull,

Penske Truck Leasing would like to change the design of the building and site plan to accommodate two separate buildings from the initial set that was submitted on March 16. I will be emailing a copy of the revised plans (site, building and calculations) along with a hard copy overnighted to your office over the next day for you to begin your preliminary intensity analysis of the new layout.

Sincerely,

Mike Barnes

Penske Truck Leasing Co. Senior Regional Facilities Manager

480-276-5888



Penske Truck Leasing 1711 W. Greentree Dr. Suite 117 Tempe, AZ 85284

Tel: 602-253-0041 Fax: 480-7754350

www.pensketruckleasing.com

March 16, 2021

Riverside County Airport Land Use Commission 40480 Lemon Street., 14th. Floor Riverside, CA 92501 Attn: Paul Rull, ALUC Principal Planner

RE: ZAP1456MA21 ALUC Review

Dear Mr Rull,

The Site Plan intended for this location includes Short Term and Long Term parking. Short Term is described as parking for everyday use by customers, employees (shown in Grey on the plan). These spaces turn over daily. Long Term parking is described as trucks and trailers parked in the stall at or greater than 10 days. This involves trucks and trailer dropped off by the customer and parked, waiting for servicing in a time frame of 10-15 days before it get picked up. This tends to be 90% or our operation while 10% for that of Short Term.

Sincerely,

Mike Barnes

Commence of the control of the contr

Penske Truck Leasing Co. Senior Regional Facilities Manager

480-276-5888



Project Narrative Moreno Valley, CA February 22, 2021

Penske Truck Leasing Co, L.P. ("Penske") is proposing to construct a new State-of-the-Art (6)-Bay Service Facility (w/7,080 sq.ft. office core and Wash Bay) and associated (2)-Lane, (3)-product (gas, diesel, DEF) Fuel Island located in the Moreno Valley, CA vicinity.

Penske's intended use of the premises is for "the operation of a motor vehicle and truck leasing, rental and sales business, including the housing, maintaining to full capacity and repairing of motor trucks and trailers, outside parking and storage of such vehicles, motor vehicle repair shop and the storage and dispensing of fuel for the benefit of our internal customers only".

We conduct four essential functions from the facility: (1) Used Truck Sales, (2) Provide Local One-way rentals to the General Public and Commercial Industry, (3) Provide Full Service Lease (FSL) or Contract Maintenance (CM) to our contractual customers and (4) Provide maintenance, fueling and washing of our supporting fleet (Truck Sales is optional at some locations and may not be applicable).

Penske will conduct truck and trailer repairs consisting of general and preventative maintenance to our private and customer fleet (not open to the general public) such as clutches, oil changes, belt/bulb replacements, tune-ups, tire changes, etc. No major work such as framework, collision repair or Body Shop work, etc. would occur. All work is conducted within the facility.

A high-level overview of our maintenance operation/product lines for our location:

- Full Service Lease (FSL) 4, 5, and 6 year leasing of commercial transportation equipment Equipment Type Semi tractors I Box Trucks I Semi Trailers. The customer rate for this equipment guarantees all of the maintenance on these units. The customer understands what the monthly costs are in one essential rate. We assume all of the maintenance expense for these units. Penske owns these Assets / we sell the units at the end of the lease agreement.
- Contract Maintenance (CM) Customer owns the unit and agrees to a set term (years of term vary).
 - Penske provides the maintenance at a contractually agreed upon labor and parts rate.
- Washing and Fueling Only Penske Vehicles and/or Customers can Wash or fuel on the premises. The location is closed to the general public for these services.

Location Information:

Penske typically operates 2 shifts, subject to change:

Service: Monday – Friday 6:00 AM to 12 AM

Saturday 7:00 AM to 4:00 PM Sunday 7:00 AM to 12:00 PM

Rental: Mon – Fri 7:00 AM to 6:00 PM

Sat 7:00 AM – 4:00 pm Sun 7:00 AM to Noon



- We anticipate employment of approximately 31 associates at this location: 4 Office staff (rental, leasing, sales) 20- Fully certified diesel technicians 7 Service staff

 - Total Associates at Facility: 31

Position	Day Shift Count (6am-3pm)	2 nd Shift Count(3pm-12am)
Tech 3	3	4
Tech 2	4	5
Tech 1	2	2
CSR	1	1
Maint Supervisor	1	1
BSM	1	
Service Admin Clerk	1	
Parts Clerk	1	
Branch Admin Clerk	1	
BRM	1	
RMT	2	
Total	18	13

NOTICE OF PUBLIC HEARING RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact <u>ALUC Planner Paul Rull at (951) 955-6893</u>. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The City of Moreno Valley Planning Department should be contacted on non-ALUC issues. For more information please contact City of Moreno Valley Planner Mr. Jeff Bradshaw at (951) 413-3224.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to prull@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please telephone Barbara Santos at (951) 955-5132.

PLACE OF HEARING: Riverside County Administration Center

4080 Lemon Street, 1st Floor Board Chambers

Riverside California

DATE OF HEARING: May 13, 2021

TIME OF HEARING: 9:30 A.M.

Pursuant to Executive Order N-25-20, this meeting will be conducted by teleconference and at the Place of Hearing, as listed above. Public access to the meeting location will be allowed, but limited to comply with the Executive Order. Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org

CASE DESCRIPTION:

ZAP1456MA21 – Penske Truck Leasing Co., L.P/CD Moreno Holding LLC. – City of Moreno Valley Case No. PEN21-0025 (Plot Plan) a proposal to construct a Penske Truck facility including a 19,200 square foot building, a 1,792 square foot building, and a 1,936 square foot truck fueling island on 9.10 acres located southerly of Alessandro Boulevard, easterly of Old 215 Frontage Road, and northerly of Cactus Avenue. Originally, a 16 building industrial business park (located partially on the proposed site) was proposed (ZAP1038MA7) and found consistent by the Commission in 2007. A revision to the project was proposed (ZAP1333MA18) that included 10 industrial warehouse buildings and a 15,280 square foot Penske Truck facility building, was found consistent by the Commission in 2018 (Airport Compatibility Zones B1-APZ-I and B1-APZ-II of the March Air Reserve Base/Inland Port Airport Influence Area).



ERSIDE COUNTY AIRPORT LAND USE COMMISSION

UC CASE NUMBER: _	ZAP1456MAZ1 DATE SUBMITTED:	02/24/2021
PLICANT / REPRESENTA	TIVE / PROPERTY OWNER CONTACT INFORMATION	
plicant	PENSKE TRUCK LEASING CO, L.P.	Phone Number 602-253-0041
iling Address	2675 MORGANTOWN ROAD	Email MIKE.BARNES@PENSKE.COM
	READING, PA 19607	
presentative	MIKE BARNES, PENSKE TRUCK LEASING CO, L.P.	Phone Number 480-276-5888
ailing Address	1711 W. GREENTREE DR. SUITE 117	Email MIKE.BARNES@PENSKE.COM
Billing Address	TEMPE, ARIZONA 85284	
	CD MORENO HOLDING LLC. ATTN: TOM K KOSS	Phone Number 949-697-2335
operty Owner	30782 HUNT CLUB DRIVE	Email TOMKKOSS@GMAIL.COM
ailing Address	SAN JUAN CAPISTRANIO, CA 92675	
taff Contact Mailing Address Local Agency Project No	JEFFREY BRADSHAW 14117 FREDERICK STREET MORENO VALLEY, CA 92553 PEN 21-0025	Case Type General Plan / Specific Plan Amendment Zoning Ordinance Amendment Subdivision Parcel Map / Tentative Tract Use Permit Site Plan Review/Plot Plan Other
PROJECT LOCATION	the distance of the avalent site to the aimort houndary and runways	
Attach an accurately scaled r Street Address	nap showing the relationship of the project site to the airport boundary and runways HIGHWAY 215 FRONTAGE ROAD	
Assessor's Parcel No.	287-120-002, 287-120-003, PORTION OF 287-100-088, 287-100-078,287-100-078,287-120-016,297-120-017, 287-120-018	Gross Parcel Size 9.102 ACRES
Subdivision Name	ALESANDRO TRACT	Nearest Airport and distance from Air-
Lot Number	3	port MARCH AIR RESERVE BASE 8,838 F
	the legation of structures open spaces and y	water bodies, and the heights of structures and trees; include a
PROJECT DESCRIPTION if applicable, attach a detail tional project description do Existing Land Use	that as needed UNDEVELOPED PROPERTY WITHOUT EXSTING	

Riverside County Airport Land Use Commission, County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, CA 92501, Phone: 951-955-5132 Fax: 951-955-5177 Website: www.rcaluc.org

Proposed Land Use (describe)	PENSKE TRUCK LEASING PROPOSES TO CONSTRUCT A 21,540 SQUARE FOOT TRUCK LEASING FACILITY WITH A COVERED TWO LANE FUEL DISPENSING AND STORAGE AREA SITE WILL BE PAVED FOR TRUCK STORAGE AND CIRCULATION			
For Residential Uses	Number of Parcels or Units on Site (exclude secondary units) Hours of Operation M-F 6AM -6PM: Set 7AM -4PM; Sun 7AM - 12PM			
For Other Land Uses	Tibula of operation	Maximum Number	31 (Total Associates Employeed)	
(See Appendix C)	Number of People on Site 20 Method of Calculation		nt between day and 2nd, shift. People count will not exceed 2	0.
		(Proj Narrative breaks down people count)		
Height Data	Site Elevation (above mean sea lev		1,540 26	ft. ft.
Flight Hazards	Height of buildings or structures (from the ground) Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight? If yes, describe			

- NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections Α. 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- REVIEW TIME: Estimated time for "staff level review" is approximately 30 days from date of submittal. В. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- SUBMISSION PACKAGE: C.
 - 1..... Completed ALUC Application Form
 - 1..... ALUC fee payment
 - 1..... Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
 - 1...... Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
 - 1.... CD with digital files of the plans (pdf)
 - 1. Vicinity Map (8.5x11)
 - 1..... Detailed project description
 - 1. Local jurisdiction project transmittal
 - 3. Gummed address labels for applicant/representative/property owner/local jurisdiction
 - 3..... Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. (Only required if the project is scheduled for a public hearing Commission meeting)

COUNTY OF RIVERSIDE AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM:

3.1

HEARING DATE:

May 13, 2021

CASE NUMBER:

ZAP1111FV21 - AZIZ, LLC (Representative: Michael

Ramirez)

APPROVING JURISDICTION:

County of Riverside

JURISDICTION CASE NO:

CZ2100009 (Change of Zone)

LAND USE PLAN:

2007 French Valley Airport Land Use Compatibility Plan, as

amended in 2011

Airport Influence Area:

French Valley Airport

Land Use Policy:

Airport Compatibility Zone E

Noise Levels:

Outside 55 CNEL contour

MAJOR ISSUES:

None

RECOMMENDATION: Staff recommends that the proposed Change of Zone be found <u>CONSISTENT</u> with the 2007 French Valley Airport Land Use Compatibility Plan, as amended in 2011.

PROJECT DESCRIPTION: A proposal to change the zoning of 2.94 gross acres from Industrial Park (I-P) to Manufacturing Service Commercial (M-SC).

A previous proposal to construct a retail and fueling facility on this site was found consistent under ZAP1096FV20 as a staff review.

PROJECT LOCATION: The proposed project is located on the northwest corner of Jean Nicholas Road and Winchester Road in the unincorporated community of French Valley, approximately 10,990 feet northeasterly of the northern terminus of Runway 18-36 at French Valley Airport.

Staff Report Page 2 of 2

BACKGROUND:

Residential Density/Non-Residential Intensity: The site is located within Zone E of the French Valley Airport Influence Area, where residential density and non-residential intensity are not restricted.

<u>Prohibited and Discouraged Uses:</u> The applicant does not propose any uses prohibited or discouraged in Compatibility Zone E (hazards to flight).

Noise: The site is located outside the 55 CNEL aircraft noise contour for French Valley Airport. Therefore, no special measures to mitigate aircraft noise are required at this location.

<u>Part 77:</u> The elevation of Runway 18-36 at its northerly terminus is 1,347 feet above mean sea level (AMSL). At a distance of approximately 10,990 feet from the runway to the site, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,456 feet AMSL. The proposed change of zone application does not propose any buildings.

The previous review of the proposed buildings under ZAP1096FV20 found that the building pad elevation is 1,396 feet AMSL, with a maximum building height of 29 feet, resulting in a top point elevation of 1,425 feet. Therefore, review of the proposed buildings by the FAA Obstruction Evaluation Service (FAA OES) was not required.

Open Area: Compatibility Zone E does not require land to be set aside as open areas, and the site is less than 10 acres in area.

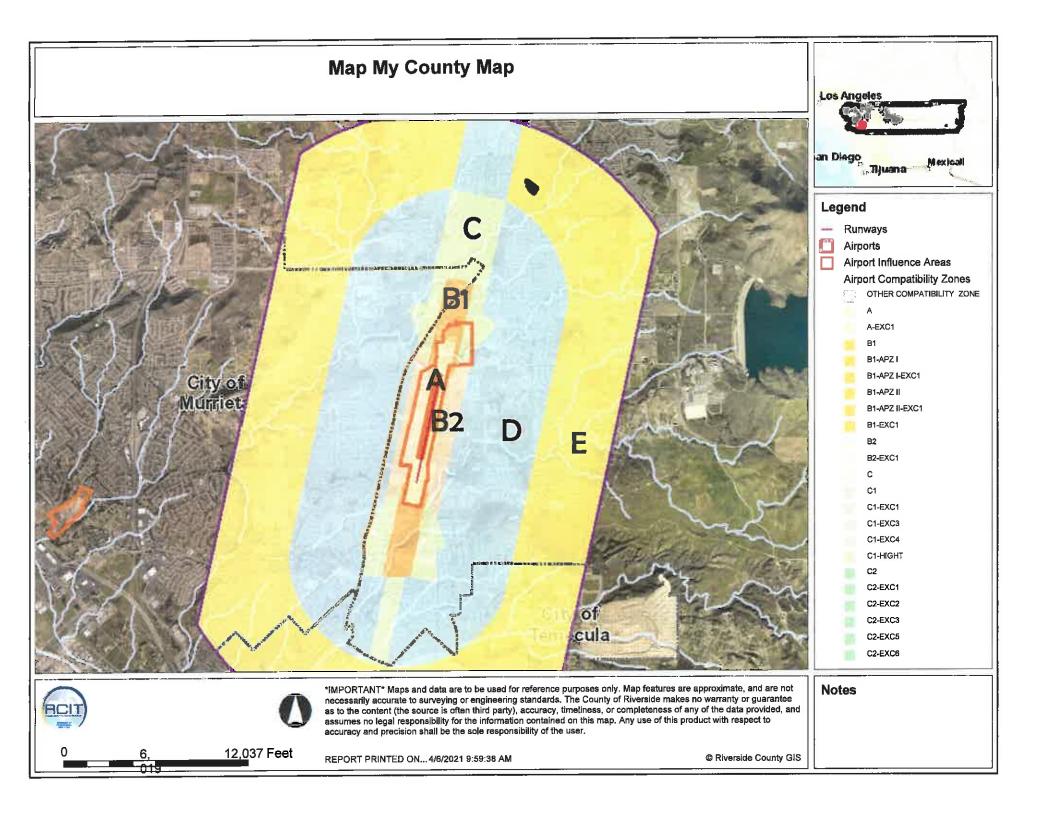
<u>Change of Zone:</u> The applicant proposes to change the site's 2.94 acres zoning from Industrial Park (I-P) to Manufacturing Service Commercial (M-SC). The proposed change of zone would be as, or more, consistent with the Compatibility Plan as long as the underlying development's is consistent with the compatibility criteria.

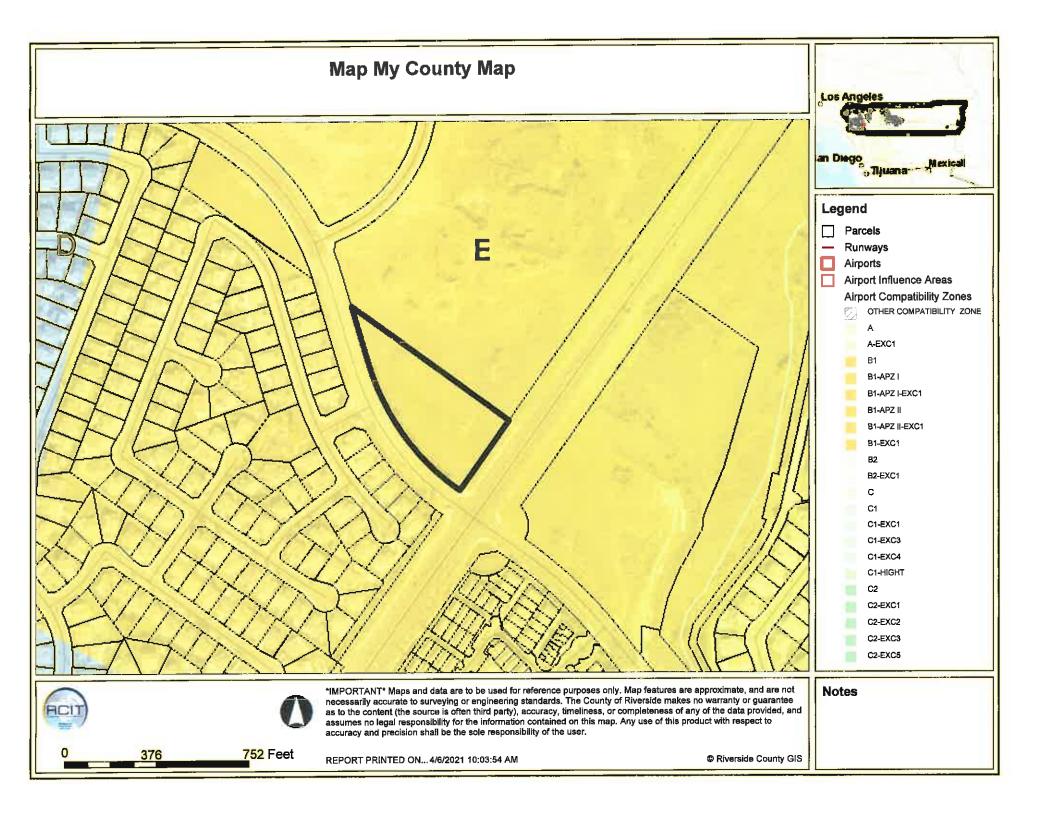
Y:\AIRPORT CASE FILES\French Valley\ZAP1111FV21\ZAP1111FV21sr.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b)











Legend

- Parcels
- --- County Centerlines **Blueline Streams**
- City Areas World Street Map





752 Feet

IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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Notes





Legend

 County Centerlines **Blueline Streams** City Areas World Street Map





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Notes

6,019 Feet

@ Riverside County GIS





Legend

- Parcels
- **County Centerlines** Blueline Streams
- City Areas World Street Map





1,505 Feet

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Notes





Legend

- Parcels
- County Centerline Names
- County Centerlines
 Blueline Streams
- City Areas
 World Street Map





"IMPORTANT" Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

188 <u>3</u>76 Feet

REPORT PRINTED ON...4/6/2021 10:09:58 AM

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Notes



NOTICE OF PUBLIC HEARING RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact <u>ALUC Planner Paul Rull at (951) 955-6893</u>. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The Riverside County Planning Department should be contacted on non-ALUC issues. For more information please contact Riverside County Planner Ms. Deborah Bradford at (951) 955-6646.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to prull@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please telephone Barbara Santos at (951) 955-5132.

PLACE OF HEARING: Riverside County Administration Center

4080 Lemon Street, 1st Floor Board Chambers

Riverside California

DATE OF HEARING: May 13, 2021

TIME OF HEARING: 9:30 A.M.

Pursuant to Executive Order N-25-20, this meeting will be conducted by teleconference and at the Place of Hearing, as listed above. Public access to the meeting location will be allowed, but limited to comply with the Executive Order. Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org

CASE DESCRIPTION:

ZAP1111FV21 – AZIZ, LLC (Representative: Michael Ramirez) – County of Riverside Case No. CZ2100009 (Change of Zone). A proposal to change the zoning of 2.94 gross acres located on the northwest corner of Jean Nicholas Road and Winchester Road from Industrial Park (I-P) to Manufacturing Service Commercial (M-SC) (A previous proposal to construct a retail and fueling facility on this site was found consistent by the ALUC) (Airport Compatibility Zone E of the French Valley Airport Influence Area).



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW ALUC CASE NUMBER: 2AC 1111 FV21 DATE SUBMITTED: APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION (858)729-3491 Phone Number **AZIZ LLC Applicant** Email mamoil.amiraziz@gmail.com 495 E Rinon Ave Ste 175 **Mailing Address** Corona, CA 92879 760-810-8548 **Phone Number** Michael Ramirez Representative Email mramirez@beyondfoodmart.com 4300 Edison Ave, Mailing Address Chino, CA 91710 (858)729-3491 AZIZ LLC **Phone Number Property Owner** mamoil.amiraziz@gmail.com 495 E Rinon Ave Ste 175 **Mailing Address** Corona, CA 92879 **LOCAL JURISDICTION AGENCY** 951-955-6646 TRANSPORTATION AND LAND MANAGEMENT AGENCY Phone Number Local Agency Name Email DBRADFOR@RIVCO.ORG DEBORAH BRADFORD Staff Contact Case Type CUP / ZONE CHANGE Mailing Address General Plan / Specific Plan Amendment Zoning Ordinance Amendment Zoning Ordinance Americana Subdivision Parcel Map / Tentative Tract Use Permit Local Agency Project No PPT210001 (FKA CUP 200002) Site Plan Review/Plot Plan CHANGE OF ZONE (2 210000) Other **PROJECT LOCATION** Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways NW corner of Jean Nicholas Rd & Winchester Rd, Winchester, CA 92596 Street Address 2.94 Acres 480-462-004 **Gross Parcel Size** Assessor's Parcel No. Nearest Airport and **Subdivision Name** distance from Air-French Valley Airport / 2 miles port Lot Number PROJECT DESCRIPTION if applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed Vacant Land - SEE ATTACHED PROJECT PLANS **Existing Land Use** (describe)

4.V

art store with QSR, an express car wash and STARBI	UCKS with drive thru	
of Parcels or Units on Site (exclude secondary units) Operation 24 Hours		
•		
ition (above mean sea level)	1398	ft,
buildings or structures (from the ground)	27 feet high	ft.
g lights, glare, smoke, or other electrical or visual hazards to	ical interference, Yes aircraft flight? No	
	of People on Site Maximum Number d of Calculation ation (above mean sea level) f buildings or structures (from the ground) project involve any characteristics which could create electr g lights, glare, smoke, or other electrical or visual hazards to	of People on Site Maximum Number If of Calculation Ation (above mean sea level) If buildings or structures (from the ground) Project involve any characteristics which could create electrical interference, glights, glare, smoke, or other electrical or visual hazards to aircraft flight? No

- A
- REVIEW TIME: Estimated time for "staff level review" is approximately 30 days from date of submittal. B. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. **SUBMISSION PACKAGE:**
 - 1. Completed ALUC Application Form
 - 1. ALUC fee payment
 - 1..... Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
 1. Plans Package (8.5x11) (site plans, floor plans, building elevations,
 - grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
 - 1..... CD with digital files of the plans (pdf)
 - 1..... Vicinity Map (8.5x11)
 - 1..... Detailed project description
 - 1..... Local jurisdiction project transmittal
 - 3. Gummed address labels for applicant/representative/property owner/local jurisdiction
 - 3. Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. (Only required if the project is scheduled for a public hearing Commission meeting)

COUNTY OF RIVERSIDE AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM: 3.2

HEARING DATE: May 13, 2021

CASE NUMBER: ZAP1106FV21 - Pierer Immoreal North America, LLC

(Representative: CASC Engineering and Consulting)

APPROVING JURISDICTION: Riverside County

JURISDICTION CASE NO: PPT180022S01 (Plot Plan Substantial Conformance)

LAND USE PLAN: 2007 French Valley Airport Land Use Compatibility Plan, as

amended in 2011

Airport Influence Area: French Valley Airport

Land Use Policy: Zones B2 and D

Noise Levels: 55-65 CNEL

MAJOR ISSUES: The project's single acre intensities are inconsistent with the Compatibility Zones B2 and D single acre intensity criteria when the building occupancy is calculated using the State's Building Code.

The applicant has provided an occupancy survey study of another similar KTM campus in the area. The data in the study reveals the most realistic conservative occupancy of 36 people for the headquarters building, and 32 people for the motorsport building, are significantly lower than the occupancy calculated for the proposed two buildings when using the Building Code method (992 people for the headquarters building, and 516 people for the motorsport building).

If we applied the survey data to the proposed project, the Zone B2 motorsport building would result in an average intensity of 3 people per acre and 32 people in a single acre, and the Zone D headquarters building would result in an average intensity of 4 people per acre and 36 people in a single acre, both of which are consistent with the Zone B2 and D average and single acre criteria.

RECOMMENDATION: Staff recommends that the Commission find the Plot Plan Substantial Conformance <u>CONDITIONALLY CONSISTENT</u>, based on the provided occupancy survey study, subject to the conditions included herein, and such additional conditions as may be required by the Federal Aviation Administration Obstruction Evaluation Service.

PROJECT DESCRIPTION: A proposal to construct a KTM motorsport research and development facility including a two-story 58,351 square foot headquarters office building, a 68,741 square foot motorsport research building, and a 23,697 square foot warehouse building (with an outdoor 20,696 square foot semi-truck parking area and an outdoor 8,602 square foot maintenance area) on 19.57 acres.

On June 13, 2019 (ZAP1083FV18) a proposal to construct a two-story 47,675 square foot headquarters office building, a 60,860 square foot motorsport research building, and a 17,917 square foot warehouse building (with an outdoor 20,696 square foot semi-truck parking area and an outdoor 8,602 square foot maintenance area) on 19.57 acres of this site was reviewed by ALUC and determined conditionally consistent. (A subsequent tentative parcel map under ZAP1100FV20 to divide 31.86 acres into two parcels was also found consistent by the Commission on June 4, 2020)

PROJECT LOCATION: The site is located easterly of Winchester Road (State Highway Route 79), westerly of Sky Canyon Road, northerly of Borel Road, and southerly of Sparkman Way, within the unincorporated community of French Valley, approximately 1,231 feet westerly of the southerly end of Runway 18-36 at French Valley Airport.

BACKGROUND:

Non-Residential Average Intensity: Pursuant to the French Valley Airport Land Use Compatibility Plan, the project site is located within Compatibility Zones B2 (10.12 acres) and D (9.46 acres). Zone B2 restricts average intensity to 100 people per acre, and Zone D restricts average intensity to 150 people per acre through French Valley Airport Compatibility Plan Policy 2.4.

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan, the following rates were used to calculate the occupancy for the proposed building in Compatibility Zones B2 and D:

- office 1 person per 200 square feet,
- manufacturing 1 person per 200 square feet,
- conference room/reception area/break area 1 person per 15 square feet,
- vocation training room 1 person per 50 square feet,
- warehouse − 1 person per 500 square feet, and
- storage 1 person per 300 square feet.

The proposed project includes a two-story 58,351 square foot headquarters office building, a 68,741 square foot motorsport research building, a 23,697 square foot warehouse building, a 20,696 square foot outdoor semi-truck parking area, and an outdoor 8,602 square foot maintenance area (which includes 13,153 square feet of office area, 36,759 square feet of manufacturing area, 16,572 square feet of warehouse area, 17,437 square feet of storage area, 15,568 square feet of conference room/reception/break area, 5,342 square feet of vocation training room, and 14 outdoor entrance bays for truck parking and maintenance), accommodating a total occupancy of 1,508 people and

Staff Report Page 3 of 8

resulting in an average intensity of 77 people per acre which is consistent with the Compatibility Zones B2 average criterion of 100 and Compatibility Zone D criterion of 150.

A breakdown of use by Compatibility Zone indicates that Zone B2 would contain the motorsport building, warehouse, and outdoor entrance bays for truck parking and maintenance, which includes 4,303 square feet of office area, 33,269 square feet of manufacturing area, 3,425 square feet of lobby/breakroom area, 16,572 square feet of warehouse area, 13,643 square feet of storage area, and 14 outdoor entrance bays for truck parking and maintenance, accommodating 516 people, resulting in an average intensity of 51 people per acre, which is consistent with the Compatibility Zone B2 average criterion of 100.

The Zone D portion of the site would contain the two-story headquarters building, which includes 8,850 square feet of office area, 3,490 square feet of manufacturing area, 12,143 square feet of conference room/reception/breakroom area, 5,342 square feet of vocation training room area, and 3,794 square feet of storage area, accommodating 992 people, resulting in an average acre intensity of 105 people, which is consistent with the Compatibility Zone D criterion of 150.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per standard vehicle). Based on the number of standard parking spaces provided of 272 spaces, the total occupancy would be estimated at 408 people, resulting in an average intensity of 21 people per acre for the entire site, which is consistent with both the Zone B2 average criterion of 100 and the Zone D criterion of 150.

Non-Residential Single-Acre Land Use Intensity: Pursuant to the French Valley Airport Land Use Compatibility Plan, the project site is located within Compatibility Zones B2 and D. Zone B2 restricts single acre intensity to a maximum of 200 people, and Zone D restricts single acre intensity to 450 people in the most intensely utilized acre, through French Valley Airport Compatibility Plan Policy 2.4.

Based on the site plan provided and the occupancies as previously noted, the single-acre intensities of concern would occur within a portion of the motorsport research and development building in Compatibility Zone B2, and entirely consist of the two-story headquarters building in Compatibility Zone D.

The single acre intensity for the motorsport building located entirely within Compatibility Zone B2 would include 4,796 square feet of office area, 17,307 square feet of manufacturing area, 5,326 square feet of storage area, and 3,425 square feet of break room/lobby area, accommodating a single acre occupancy of 357 people, which is inconsistent with the Compatibility Zone B2 criterion of 200.

The single acre intensity for the two-story headquarters building located entirely within Compatibility Zone D would include 8,850 square feet of office area, 3,490 square feet of manufacturing area, 3,794 square feet of storage area, 12,143 square feet of conference room/lobby area, and 5,342 square feet of vocational training area, accommodating a single-acre occupancy of

Staff Report Page 4 of 8

992 people, which is inconsistent with the Compatibility Zone D criterion of 450.

<u>KTM North America Campus – Occupancy Survey of Similar Facility</u>: In light of the project's inconsistent single acre intensities (per the Building Code Method) in Zones B2 and D, the applicant prepared a survey study examining the occupancies of a similar campus.

The study highlights the fact that the building code method used for determining site intensity is a conservative approach in calculating actual project intensity. As such, the study examines one similar existing campus facility, spread over 4 separate buildings located at 38415 Innovation Court, 38429 Innovation Court, 38340 Innovation Court, and 30100 Technology Drive, Murrieta, for a more realistic occupancy generation calculation.

The study examines swipe card data collected between October 1, 2019 and December 31, 2019, identifying the following:

- highest daily total of 461 people for the entire (4 building) campus facility,
- highest daily total of 227 people for just the 2-building headquarters, and
- highest daily total of 234 people for just the 2-building motorsports.

Additionally, the study also provides highest hourly totals (occurring on December 17, 2019, at 11:00 a.m.), identifying the following:

- highest hourly total of 68 people for the entire (4 building) campus facility,
- highest hourly total of 36 people for just the 2-building headquarters,
- highest hourly total of 32 people for just the 2-building motorsports.

All of these results are significantly lower compared to the occupancy calculated using the Building Code method of 1,508 people for the proposed KTM facility (516 people for the motorsport building, 992 people for the headquarter building).

If we applied the highest hourly total from the study of 36 people for the headquarter buildings and 32 people for the motorsport buildings, the proposed project would accommodate a total occupancy of 68 people. The Zone B2 motorsport building would result in an average intensity of 3 people per acre and 32 people in a single acre which are consistent with the Zone B2 average criteria of 100 people per acre and single acre criteria maximum of 200 people. The Zone D headquarters building would result in an average intensity of 4 people per acre and 36 people in a single acre which are consistent with the Zone D average criteria of 150 people per acre and single acre criteria maximum of 450 people.

It is important to note that the existing 81,723 square foot KTM campus facility is significantly smaller than the proposed 150,780 square foot KTM project. However, if we were to double the building size and occupancy of the existing facility, the resulting 163,446 square foot facility (which is more comparable to the proposed KTM project size) would result in a total occupancy of 136 people, 72 people in the headquarters building, and 64 people in the motorsports building. The resulting intensities would still be consistent with the Zones B2 and D average and single acre intensity criteria.

<u>Prohibited and Discouraged Uses:</u> The applicant does not propose any uses prohibited or discouraged in Compatibility Zones B2 or D (children's schools, day care centers, libraries, hospitals, nursing homes, places of worship, highly noise-sensitive outdoor non-residential uses, hazardous materials and hazards to flight), other than the proposed prohibited nonresidential intensity.

Noise: The French Valley Compatibility Plan depicts the site as being located within the 55-65 CNEL contour range from aircraft noise. Office and industrial uses are identified as normally and marginally acceptable within the 55-65 CNEL contour range. The indoor sensitive uses like office areas would be impacted by aircraft generated noise, and, therefore, staff is recommending a condition to incorporate noise attenuation measures into the design of these areas to such extent as may be required to ensure that interior noise levels from aircraft operations are at or below 45 CNEL.

Part 77: The elevation of Runway 18-36 at its southerly terminus is 1,340 feet above mean sea level (AMSL). At a distance of approximately 1,231 feet from the runway to the project boundary, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,352.3 feet AMSL.

The original project's finished floor elevation was 1,327.5 feet AMSL. With a maximum building height of 29 feet, the top point elevation was 1,356.5 feet. Therefore, review of the buildings by the FAA Obstruction Evaluation Service (FAAOES) was required. The applicant submitted to the FAAOES and Aeronautical Study Numbers 2018-AWP-15606-OE (motorsport building), 2018-AWP-15608-OE (headquarters building), and 2019-AWP-5221-OE (warehouse) were assigned to the project. The FAA OES issued "Determination of No Hazard to Air Navigation" letters dated November 1 and 2, 2018 for the motorsport building and headquarter building, and July 11, 2019 for the warehouse building.

As part of the revised project, the applicant is proposing to increase the height of the headquarters building from 29 feet to 37 feet (There are no changes to the motorsport building height, therefore the original FAA no hazard determination letter 2018-AWP-15606 is still valid). The applicant submitted Form 7460-1 to the FAA OES for the headquarters building, and the FAA OES assigned Aeronautical Study Number 2020-AWP-5214-OE to this proposal. The aeronautical study revealed that the proposed structure would not exceed obstruction standards and would not be a hazard to air navigation, provided conditions are met. Therefore, FAA OES issued a "Determination of No Hazard to Air Navigation" letter on January 8, 2021.

The applicant is also proposing an increase in height for the warehouse building from 26 feet to 30 feet. The applicant has submitted a new FAA application for the increase warehouse building height. The FAA OES assigned Aeronautical Study Number 2021-AWP-5927-OE to this proposal. At the time the staff report was written, no determination has been made, but the study is in a "Work in Progress" status.

Open Area: The project site is split between Compatibility Zones B2 and D, with approximately 10.12 acres in Zone B2 and 9.46 acres in Zone D. Compatibility Zone D requires 10% (9.46 acres) of the land area within major projects (10 acres or larger) be set aside as open area that could potentially serve as emergency landing areas (Compatibility Zone B2 does not require any amount of open area due to its proximity to the actual runway.) The original project identified 2.46 acres of ALUC eligible open areas in Zone D consisting of driveway aisles and parking lot areas within the proposed development. The project is conditioned to maintain these areas consistent with ALUC open area requirements of 300 feet by 75 feet minimum shape and prohibit obstructions greater than 4 feet in height that are at least 4 inches in diameter. The proposed project revisions will not impact the original project's provision of open space area.

Hazards to Flight: Land use practices that attract or sustain hazardous wildlife populations on or near airports significantly increase the potential of Bird Aircraft Strike Hazards (BASH). The FAA strongly recommends that storm water management systems located within 5,000 or 10,000 feet of the Airport Operations Area, depending on the type of aircraft, be designed and operated so as not to create above-ground standing water. To facilitate the control of hazardous wildlife, the FAA recommends the use of steep-sided, rip-rap lined, narrow, linearly shaped water detention basins. All vegetation in and around detention basins that provide food or cover for hazardous wildlife should be eliminated. (FAA Advisory Circular 5200-33B).

The nearest portion of the project is located 1,231 feet from the runway, and therefore would be subject to the above requirement. The project would include a bioinfiltration basin, located entirely within Zone D. Such basins are to be avoided in Zone D due to the potential that such areas could provide food, water, and shelter for hazardous wildlife. Pursuant to the study "Wildlife Hazard Management at Riverside County Airports: Background and Policy", October 2018, by Mead & Hunt, which is the basis of the brochure titled "Airports, Wildlife and Stormwater Management", such basins are to be avoided in Zones D and E, unless they provide for a 48-hour drawdown and propose landscaping that is not attractive to hazardous wildlife. The project has been conditioned to be consistent with these standards in order to reduce bird attractant: 1) new basins are to be designed so as to provide for a maximum 48-hour detention period following the conclusion of a storm event, and to remain totally dry between rainfalls, and 2) any landscaping proposed in the detention basin shall be in accordance

CONDITIONS:

- 1. Any outdoor lighting installed shall be hooded or shielded to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
- 2. The following uses shall be prohibited:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight

- final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
- (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
- (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, outdoor production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
- (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
- (e) Children's schools, day care centers, libraries, hospitals, nursing homes, places of worship, buildings with more than two aboveground habitable floors, critical community infrastructure facilities, and aboveground bulk storage of 6,000 gallons or more of flammable or hazardous materials.
- (f) Highly noise-sensitive outdoor nonresidential uses.
- (g) Any use which results in a hazard to flight, including physical (e.g., tall objects), visual, and electronic forms of interference with the safety of aircraft operations.
- 3. Prior to issuance of building permits, the property owner shall convey an avigation easement to French Valley Airport. Copies of the recorded avigation easement shall be forwarded to the Airport Land Use Commission and to the County of Riverside.
- 4. The attached notice shall be provided to all prospective purchasers of the property and future tenants of the buildings thereon.
- 5. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the stormwater basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the

"AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

- 6. This project has been evaluated as consisting of 13,153 square feet of office area, 36,759 square feet of manufacturing area, 16,572 square feet of warehouse area, 17,437 square feet of storage area, 15,568 square feet of conference room/reception/break area, 5,342 square feet of vocation training room, and 14 outdoor entrance bays for truck parking and maintenance. Any increase in building area, height, change in use to any higher intensity use, change in building location or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.
- Noise attenuation measures shall be incorporated into the design of the buildings, to the extent such measures are necessary to ensure that interior noise levels from aircraft operations are at or below 45 CNEL.
- 8. The ALUC open areas as shown on the site plan shall be devoid of obstacles/obstructions greater than 4 feet in height that are at least 4 inches in diameter, which includes parking light poles, walls, trash enclosures, and tall landscaping.
- The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission and Riverside County as owner and operator of French Valley Airport. In the event of any reasonable complaint about glare related to aircraft operations, the applicant shall agree to such specific mitigation measures as determined or requested by Riverside County.

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b)

NOTICE

THERE IS AN AIRPORT NEARBY.

THIS STORM WATER BASIN IS DESIGNED TO HOLD

STORM WATER FOR ONLY 48 HOURS AND

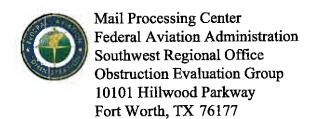
NOT TO ATTRACT BIRDS

PROPER MAINTENANCE IS NECESSARY TO AVOID BIRD STRIKES



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Aeronautical Study No. 2020-AWP-12888-OE Prior Study No. 2019-AWP-5214-OE

Issued Date: 01/08/2021

Cheryl Webb Pierer Immoreal North America 38429 Innovation Court Murrieta, CA 92563

** DETERMINATION OF NO HAZARD TO AIR NAVIGATION **

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Building KTM Headquarters - Revised 11.18.2020

Location: Murrieta, CA

Latitude: 33-34-17.05N NAD 83

Longitude: 117-07-59.66W

Heights: 1320 feet site elevation (SE)

40 feet above ground level (AGL)

1360 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

	At least 10 days prior to start of construction (7460-2, Part 1)
X	Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 07/08/2022 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (206) 231-2877, or Nicholas.Sanders@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2020-AWP-12888-OE.

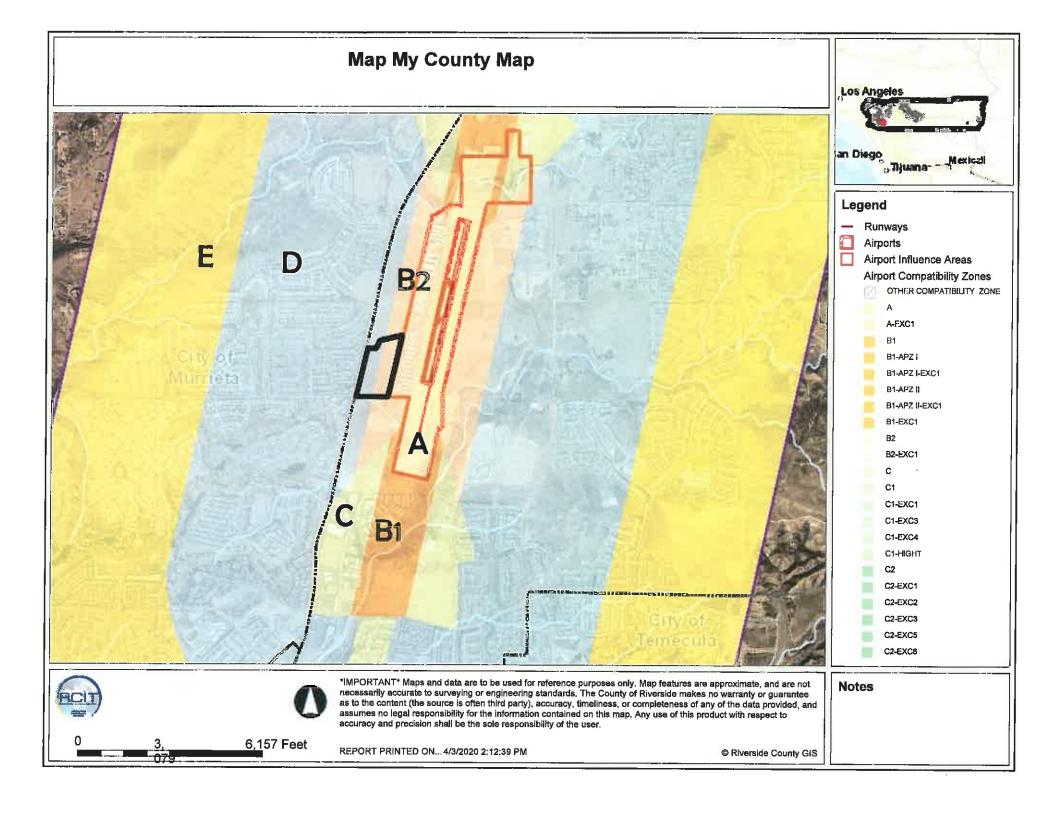
Signature Control No: 456970422-462969602 (DNE)

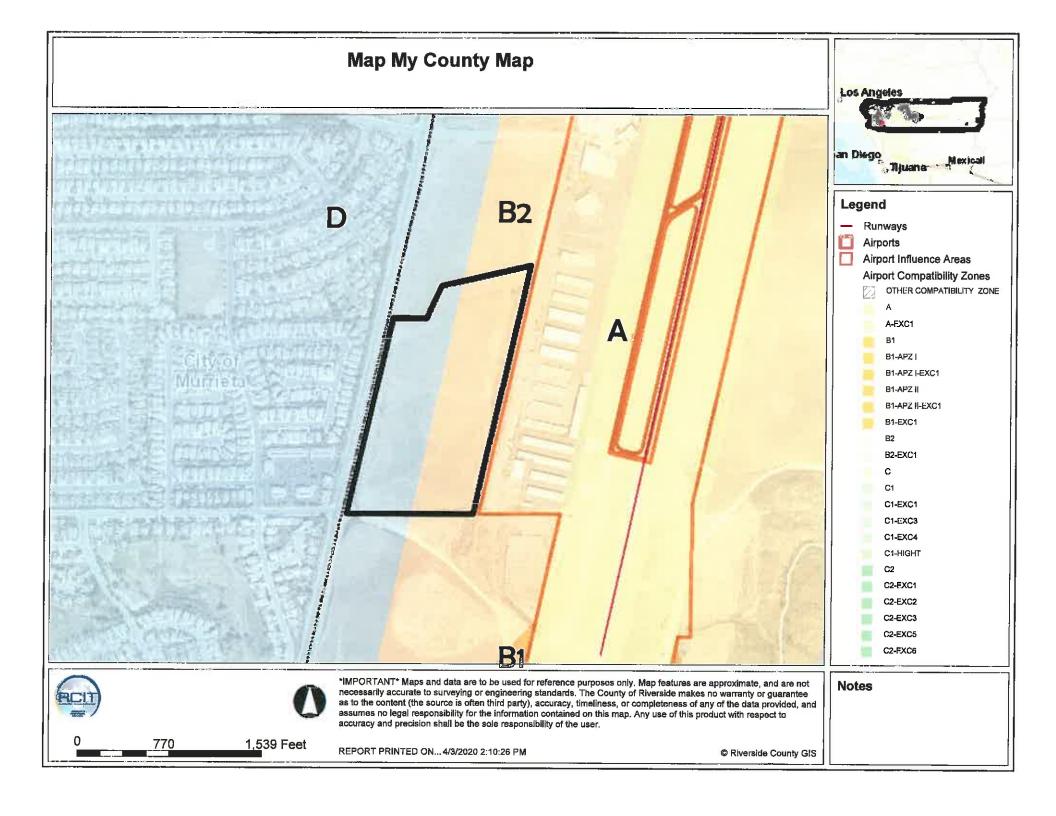
Nicholas Sanders Technician

Attachment(s) Map(s)

TOPO Map for ASN 2020-AWP-12888-OE











Legend

Blueline Streams City Areas

World Street Map





IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

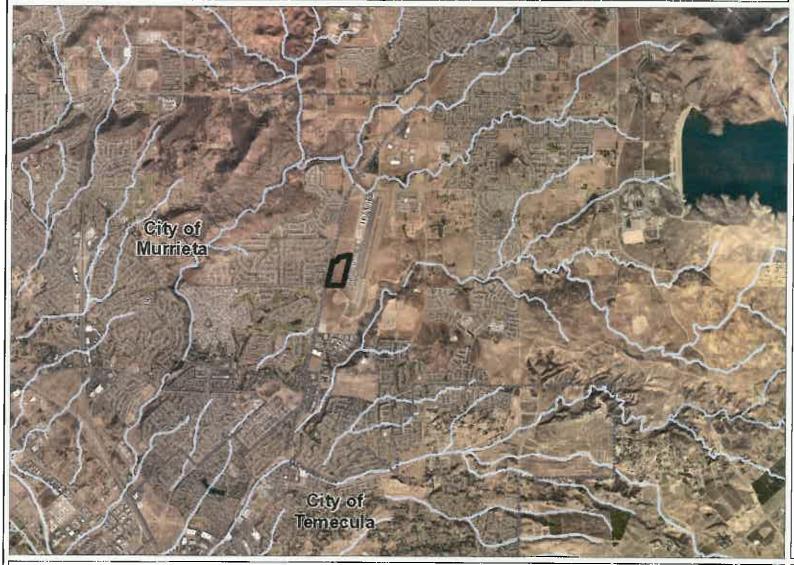
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Notes

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Map My County Map





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Blueline Streams

City Areas
World Street Map





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Notes

Map My County Map





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Blueline Streams City Areas

World Street Map





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Fax: 909.783.0108

To: Paul Ruli, ALUC Director

Riverside County Airport Land Use Commission (ALUC)

From: Frank Coyle, Director of Planning

Date: April 19, 2021

Re: ALUC Case ZAP1106FV21, KTM North America Campus - Survey of Similar Uses

CASC understands that the Riverside Airport Land Use Commission (ALUC) will be evaluating changes to the Pierer Immoreal North America, LLC (PINA, Applicant) Project (Project) which was originally approved by the Riverside Airport Land Use Commission (ALUC) on June 13, 2019 (ZAP1100FV20). We have reviewed the Project changes and based on a detailed Survey of Similar Uses prepared for the Project, the proposed modifications to the Project are consistent with the French Valley Land Use Compatibility Plan.

Project Background

On June 13, 2019, ALUC determined that the original KTM North America campus, ¹ consisting of a main Headquarters (47,675 sq. ft.), Motorsport (60,860 sq. ft.), and Storage (17,917 sq. ft.) buildings for a total building square footage of 126,452 sq. ft., was consistent with the 2007 French Valley Airport Land Use Compatibility Plan. At this time, the Applicant desires to increase the building square footage of the three buildings and to add certain outdoor facilities. The Applicant is processing an application for Substantial Conformance to the approved Plot Plan for the original Project with the Riverside County Planning Department. The Planning Department is currently reviewing the application.

The proposed modifications to the Project will result in the development of an office building complex with a gross square footage of 150,789 square feet on 31.86 acres. The three buildings proposed include a main Headquarters building of 58,351 square feet, a Motorsport building of 68,741 square feet, and a Storage building of 23,697 square feet. Additionally, 20,696 square feet is proposed for open semi-truck parking, 8,602 square feet is proposed for a maintenance intake area, and 1,300 square feet is proposed for open tire storage. The size of the proposed modified project is larger than the existing facility to only allow more space for storage, equipment, work areas, and single person offices. The increase in gross square feet is proposed to increase the efficiency and functionality of the current workforce. The buildings will not experience an increase in employees because of the increase in size.

The Project is located at 37862 Winchester Road, Murrieta, California 92563. The property is identified as Accessor's Parcel Number (APN) 963-030-013 (formerly APN 963-030-002). The property has an underlying land use designation of Commercial Office/Commercial Retail and a zoning classification of Specific Plan (SP) Borel Airpark Center (SP No. 265).

Site Intensity

Determining the site intensity for the Airport Land Use Commission (ALUC) is typically calculated using the building code method pursuant to *Appendix C*, *Table C-1*, of the Riverside County ALUC Plan. However, as made clear in *Appendix C*, utilization of the building code method may in some cases overestimate potential intensity levels compared to actual occupancy levels. The Project site is situated within

¹ KTM is an affiliate of PINA



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Compatibility Zone B2 and Compatibility Zone D of the French Valley Airport Land Use Compatibility Plan. Compatibility Zone B2 identifies an Average Acre requirement of 100 people per acre and a Single Acre requirement of 250 people per acre. Compatibility Zone D identifies an Average Acre requirement of 150 people per acre and a Single Acre requirement of 450 people per acre. Based on the building code method, the Motorsports building exceeds the single-acre intensity.

Alternative methods for determining site intensity include calculations based on parking ordinance and Survey of Similar Uses. KTM currently operates a portion of their business out of four (4) existing buildings with a gross square footage of approximately 81,723 square feet located on Innovation Court and Technology Drive, within one mile of the proposed Project. The current business operation has 138 employees. A satellite image of the current KTM Facilities is attached here as Exhibit "A". The proposed KTM campus would include up to 175 employees and 25 dealer technicians. A chart summarizing the existing facility is set forth below.

Table A, Existing Operation on Innovation Court and Technology Drive

Building No.	Address	Current Use	Gross Square Footage Used	Equivalent Use
1	38415 Innovation Court (Suite A-N), Murrieta, CA, 92563	Tech Center, Marketing, RMC Team, Media, Cust. Serv., Dealer Tech Serv., Recon, HQV Off Road, Storage	36,120 sq. ft.	Headquarters Building
2	30100 Technology Drive, Murrieta, CA, 92563	Group HQ Executive Conference Rooms, Sales, HQV SX/MX Offices, Warehouse S&R, Parts Storage, Acct/Finance, Legal, Inside Sales	15,100 sq. ft.	Headquarters Building
		Gross Square Footage	51,220	sq. ft.
3	38429 Innovation Court, Murrieta, CA, 92563	KTM Motorsports, Dealer Development, E-bike Team offices, Parts Storage, Homologation Offices, Human Resources, Executive Offices	21, 504 sq. ft.	Motorsports Building
4	38340 Innovation Court (Suite 701-703), Murrieta, CA, 92563	R&D, WP, Parts Equip, Storage	8,999 sq. ft.	Motorsports Building
		Gross Square Footage	30,50	3 sq. ft.

As shown in **Table A**, Buildings 1 through 4 are sorted into two categories, Headquarters and Motorsports, which represent the future buildings uses for the modified project. Building numbers 1 and 2 serve the same use as the proposed Headquarters building, and building numbers 3 and 4 serve the same purpose

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as the proposed Motorsports building. The 138 employees who currently work out of Buildings 1 through 4 would work out of the two buildings proposed for the modified Project.

Table B, Comparison of Development, shows a side by side comparison of the distribution of square feet currently used for Headquarters and Motorsports use, the gross size of the original development plan previously approved by ALUC, and the gross size of the proposed modified project for consideration by ALUC now.

Table B, Comparison of Development

Building	Existing Facility	Original Project ZAP1106FV20	Modified Project ZAP1106FV21
Headquarters	51,220 sq. ft.	47,675 sq. ft.	58,351 sq. ft.
Motorsports	30,503 sq. ft.	60,860 sq. ft.	68,741 sq. ft.
Storage	-	17,917 sq. ft.	23,697 sq. ft.
Gross Square Footage	81,723 sq. ft.	126,452 sq. ft.	150,789 sq. ft.

Survey of Similar Uses

To prepare a Survey of Similar Uses for the proposed Project, the Applicant has analyzed three months of data from "swipe card readers" on the four Innovation Court buildings KTM currently operates with its existing workforce of 138 employees. As shown above in **Table A**, the four existing buildings serve the Applicant for two uses, Headquarters and Motorsports. To provide an accurate projection of the number of employees to the proposed Headquarters and Motorsports buildings on APN 963-030-013, **Table C** categorizes the data from four buildings into the respective two categories.

This study analyzes foot traffic during a three month time period, October 1, 2019 through December 31, 2019, ("Study Period") as this is the busiest time of the year for employees to the facility. **Table C** demonstrates that during the busiest quarter of the year, the Headquarters building experiences a peak daily average of 26 employees throughout the day, and the Motorsports building experiences a peak daily average of 27 employees throughout the day. Overall, on any given weekday, there are no more than an average of 53 employees out of the existing 138 employees on the campus in a day. Thus, there are no more than 38% of the employees on the campus during the busiest time of the week.

Table C, Average Number of Employees per Weekday

	Average Daily Number of Employees on Existing Campus					
Day of Week	Headquarters	Motorsports	Total			
Monday	24	27	51			
Tuesday	26	27	53			
Wednesday	26	25	51			
Thursday	24	23	47			
Friday	21	19	40			
Saturday	1	1	2			
Sunday	0	1	1			

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Swipe data is registered with a date and time stamp, along with an employee "Badge" number to identify who is entering each building and at what time of the day. The "worst case scenario" is derived from the peak number of employees to visit the buildings in one hour on any day in the Study Period. The day with the "worst case scenario" is December 17, 2019, where 68 employees were present during the 11:00 AM to 12:00 PM hour. As shown below, the Headquarters building experienced a peak hourly average of 36 employees on the busiest day, and the Motorsports building experienced a peak hourly average of 32 employees on the busiest day. Overall, for any given hour, there were no more than 68 employees on campus during a peak period. Thus, there was no more than 50% of employees on campus during a peak hour.

Table D, Worst Case Scenario - Existing Facility Peak Employees per Hour

		Hourly Headcount				
Date	Hour Starting	Headquarters	Motorsports	Total		
12/17/2019	12:00 AM	0	0	0		
12/17/2019	1:00 AM	0	0	0		
12/17/2019	2:00 AM	0	0	0		
12/17/2019	3:00 AM	0	0	0		
12/17/2019	4:00 AM	0	0	0		
12/17/2019	5:00 AM	0	0	0		
12/17/2019	6:00 AM	0	0	0		
12/17/2019	7:00 AM	0	0	0		
12/17/2019	8:00 AM	1	1	2		
12/17/2019	9:00 AM	5	9	14		
12/17/2019	10:00 AM	25	29	54		
12/17/2019	11:00 AM	36	32	68		
12/17/2019	12:00 PM	22	20	42		
12/17/2019	1:00 PM	10	18	28		
12/17/2019	2:00 PM	12	17	29		
12/17/2019	3:00 PM	21	23	44		
12/17/2019	4:00 PM	29	27	56		
12/17/2019	5:00 PM	17	25	42		
12/17/2019	6:00 PM	14	25	39		
12/17/2019	7:00 PM	7	15	22		
12/17/2019	8:00 PM	8	5	13		
12/17/2019	9:00 PM	2	2	4		
12/17/2019	10:00 PM	1	0	1		
12/17/2019	11:00 PM	0	0	0		

Average Acre and Single Acre Intensity Calculations

As noted above, there are no more than 38% of employees on the existing KTM campus during a typical day. The "worst case scenario" shows that no more than 50% of employees were present on the campus in one hour. The Project as proposed will have up to 175 employees. As the existing building uses will be the same for the proposed Project, the same occupancy ratios can be applied to the proposed Project.



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Thus, there would be no more than 68 out of 175 employees on the campus during a typical day and no more than 88 out of 175 employees during a peak hour.

Table E shows the size of each building and the number of people assigned to work there. The number of employees that would be on the campus is calculated using the percentage of current employees during the "worst case scenario" peak hour which is 50%.

Table E, Average Acre and Single Acre Intensity Compatibility

Building	Square Footage	Compatibility Zone	Average Acre Limit	Single Acre Limit	Number of Employees	Worst Case Scenario Occupancy Rate	Peak # of Employees Onsite ¹
Headquarters	58,351	D	100	300	100		50
Motorsports	68,741	B2	100	200	75	50%	38
				Total	175		88

As illustrated in **Table E** above, the intensity of employees in any building or area would not exceed the Average Acre or Single Acre intensities at any time based on the conclusions as noted above. The intensity of employees is much lower than the Average and Single Acre Limits. Based on the "worst case scenario", the intensities would not be exceeded unless KTM employs over 400 employees, which is not in their business plan to have than many employees. Based on the nature of the business and the development proposed, this growth is not anticipated and the Project as proposed would be unable to support that many employees. The proposed offices and workspaces have been designed to support up to 175 employees.

The Motorsports building proposed in Compatibility Zone B2 has a more conservative Single Acre Limit of 200 people per acre. There is additional certainty that this limit will not be exceeded in any event because the Motorsport building requires a high security clearance for entry which is only available to upper management, technicians, and race teams. This includes up to 75 employees. Access is extremely limited by design and other staff would typically not be in this building at any given time. Thus, the proposed workforce of 175 employees and the proposed building square footage modifications would be compatible with the French Valley Airport Land Use Compatibility Plan and the use of the proposed Project is consistent with the Riverside ALUC Plan.

Conclusion

The Survey of Similar Uses Method reflects that the maximum number of employees on-site will be significantly lower than the Average Acre and Single Acre intensities mandated by Compatibility Zone B2 and Compatibility Zone D (See Table D). As the buildings are not open to the public, the number of personnel at the facility is predictable and consistent. The analysis of the existing facility shows that the highest number of employees to enter a building was approximately 53 people on an average weekday.



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The "worst case scenario" is represented by busiest day in the Study Period, Tuesday, December 17, 2019, where the peak number of employees per hour was 68 employees, or 50% of the workforce, from 11:00 AM to 12:00 PM. The "worst case scenario" for the proposed Headquarters building (100 employees) would be 46 employees during the peak hour, and the "worst case scenario" for the proposed Motorsports building (75 employees) would be 41 employees during the peak hour. Under the "worst case scenario", only 87 employees would be in either the Headquarters or Motorsport building. Due to the nature of the business, under no circumstances would all 87 people be in one building. Even if all 87 employees were to visit one building, the Average Acre Limits and Single Acre Limits would not be exceeded. Based on the conclusions in this analysis, we find the Project consistent with the Riverside ALUC Plan and the Average Acre and Single Acre Limits will not be exceeded.



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Exhibit "A"



Employee Visits per Day

October 1st, 2019 - December 31st, 2019

Notes:

The summary below represents the "Door Swipe" events by unique Employee Badge # for each day in the Study Period. Each day represents the total number of employees to visit the facility on that day. This data does not represent the total number of employees to be in a building at the same time.

Date	Year	Total # of	Weekday	Weekday #	Busiest Day?
12/31/2019	2010	Employees			
12/31/2019	2019 2019	35 76	Tuesday	2	FALSE
12/29/2019	2019	5	Monday	7	FALSE FALSE
12/29/2019	2019	8	Sunday	6	
12/27/2019	2019	65	Saturday	5	FALSE
12/26/2019	2019	66	Friday	4	FALSE FALSE
12/25/2019	2019	2	Thursday	3	
12/24/2019	2019	14	Wednesday Tuesday	2	FALSE FALSE
12/23/2019	2019	85	Monday	1	FALSE
12/22/2019	2019	7	Sunday	7	FALSE
12/21/2019	2019	9	Saturday	6	FALSE
12/20/2019	2019	116	Friday	5	FALSE
12/19/2019	2019	127	Thursday	4	FALSE
12/18/2019	2019	133	Wednesday	3	TRUE
12/17/2019	2019	131	Tuesday	2	FALSE
12/16/2019	2019	119	Monday	1	FALSE
12/15/2019	2019	2	Sunday	7	FALSE
12/14/2019	2019	3	Saturday	6	FALSE
12/13/2019	2019	110	Friday	5	FALSE
12/12/2019	2019	124	Thursday	4	FALSE
12/11/2019	2019	129	Wednesday	3	FALSE
12/10/2019	2019	128	Tuesday	2	FALSE
12/9/2019	2019	122	Monday	1	FALSE
12/8/2019	2019	3	Sunday	7	FALSE
12/7/2019	2019	9	Saturday	6	FALSE
12/6/2019	2019	90	Friday	5	FALSE
12/5/2019	2019	119	Thursday	4	FALSE
12/4/2019	2019	123	Wednesday	3	FALSE
12/3/2019	2019	127	Tuesday	2	FALSE
12/2/2019	2019	118	Monday	1	FALSE
12/1/2019	2019	4	Sunday	7	FALSE
11/30/2019	2019	2	Saturday	6	FALSE
11/29/2019	2019	11	Friday	5	FALSE
11/28/2019	2019	1	Thursday	4	FALSE
11/27/2019	2019	85	Wednesday	3	FALSE
11/26/2019	2019	111	Tuesday		FALSE
11/25/2019	2019	103	Monday	2	FALSE
11/24/2019	2019	3	Sunday	7	FALSE
11/23/2019	2019	5	Saturday	6	FALSE
11/22/2019	2019	73	Friday	5	FALSE
11/21/2019	2019	116	Thursday	. 4	FALSE
11/20/2019	2019	123	Wednesday	3	FALSE
11/19/2019	2019	113	Tuesday	2	FALSE
11/18/2019	2019	98	Monday	1	FALSE
11/17/2019	2019	2	Sunday	7	FALSE

11/16/2019	2019	5	Saturday	6	FALSE
11/15/2019	2019	84	Friday	5	FALSE
11/14/2019	2019	101	Thursday	4	FALSE
11/13/2019	2019	110	Wednesday	3	FALSE
11/12/2019	2019	102	Tuesday	2	FALSE
11/11/2019	2019	113	Monday	1	FALSE
11/10/2019	2019	3	Sunday	7	FALSE
11/9/2019	2019	4	Saturday	6	FALSE
11/8/2019	2019	99	Friday	5	FALSE
11/7/2019	2019	109	Thursday	4	FALSE
11/6/2019	2019	107	Wednesday	3	FALSE
11/5/2019	2019	108	Tuesday	2	FALSE
11/4/2019	2019	116	Monday	1	FALSE
11/3/2019	2019	1	Sunday	7	FALSE
11/2/2019	2019	2	Saturday	6	FALSE
11/1/2019	2019	115	Friday	5	FALSE
10/31/2019	2019	119	Thursday	4	FALSE
10/30/2019	2019	109	Wednesday	3	FALSE
10/29/2019	2019	114	Tuesday	2	FALSE
10/28/2019	2019	99	Monday	1	FALSE
10/27/2019	2019	2	Sunday	7	FALSE
10/26/2019	2019	2	Saturday	6	FALSE
10/25/2019	2019	78	Friday	5	FALSE
10/24/2019	2019	106	Thursday	4	FALSE
10/23/2019	2019	104	Wednesday	3	FALSE
10/22/2019	2019	115	Tuesday	2	FALSE
10/21/2019	2019	111	Monday	1	FALSE
10/20/2019	2019	2	Sunday	7	FALSE
10/19/2019	2019	3	Saturday	6	FALSE
10/18/2019	2019	69	Friday	5	FALSE
10/17/2019	2019	103	Thursday	4	FALSE
10/16/2019	2019	112	Wednesday	3	FALSE
10/15/2019	2019	116	Tuesday	2	FALSE
10/14/2019	2019	105	Monday	1	FALSE
10/13/2019	2019	5	Sunday	7	FALSE
10/12/2019	2019	4	Saturday	6	FALSE
10/11/2019	2019	104	Friday	5	FALSE
10/10/2019	2019	112	Thursday	4	FALSE
10/9/2019	2019	117	Wednesday	3	FALSE
10/8/2019	2019	118	Tuesday	2	FALSE
10/7/2019	2019	98	Monday	1	FALSE
10/6/2019	2019	4	Sunday	7	FALSE
10/5/2019	2019	6	Saturday	6	FALSE
10/4/2019	2019	101	Friday	5	FALSE
10/3/2019	2019	117	Thursday	4	FALSE
10/2/2019	2019	116	Wednesday	3	FALSE
10/1/2019	2019	108	Tuesday	2	FALSE
10/1/2013	2010	100	ucaudy	4	IALUE

Door Swipes per Day (includes repeat Badge swipes)

October 1st, 2019 - December 31st, 2019

Notes:

The summary below represents the "Door Swipe" events by personnel per building for each day in the Study Period. This data does not represent the total number of personnel visiting the buildings in a day; it counts the number of Door Swipes for each building which consists of multiple entries for a unique Employee Badge # as an employee enters and leaves buildings repeatedly. Thus, there are hundreds of door swipes per building in a day as personnel come and go. The intention of this dataset is to show the busiest day in the Study Period. Additional analysis is located in Appendix C which shows the headcount per hour for each building on the busiest day (worst case scenario). When referencing the full dataset in Appendix D, the Headquarters Building category represents the "KTM" building. The Motorsports Building represents the Husqvarna and Training buildings.

Date	Day	Headquarters Building	Motorsports Building	Total Swipes	Busiest Day?
10/1/2019	2	155	195	350	FALSE
10/2/2019	3	206	176	382	FALSE
10/3/2019	4	155	171	326	FALSE
10/4/2019	5	156	157	313	FALSE
10/5/2019	6	3	6	9	FALSE
10/6/2019	7	3	4	7	FALSE
10/7/2019	1	142	152	294	FALSE
10/8/2019	2	171	202	373	FALSE
10/9/2019	3	163	190	353	FALSE
10/10/2019	4	162	225	387	FALSE
10/11/2019	5	150	161	311	FALSE
10/12/2019	6	2	5	7	FALSE
10/13/2019	7	3	5	8	FALSE
10/14/2019	1	137	188	325	FALSE
10/15/2019	2	142	197	339	FALSE
10/16/2019	3	181	210	391	
10/17/2019	4	146	180	326	FALSE FALSE
10/18/2019	5	106	92		
10/19/2019	6	4	3	198	FALSE
10/20/2019	7	1	2		FALSE
10/20/2019	-1	153	168	3 321	FALSE
10/22/2019	2	166			FALSE
10/23/2019	3		218	384	FALSE
10/24/2019	4	155	162	317	FALSE
10/25/2019	5	184	198	382	FALSE
		152	153	305	FALSE
10/26/2019	6	2	6	8	FALSE
10/27/2019	7	1	8	9	FALSE
10/28/2019	1	185	175	360	FALSE
10/29/2019	2	176	207	383	FALSE
10/30/2019	3	182	213	395	FALSE
10/31/2019	4	219	214	433	FALSE
11/1/2019	5	178	184	362	FALSE
11/2/2019	6	2	6	8	FALSE
11/3/2019	7	0	5	5	FALSE
11/4/2019	1	161	194	355	FALSE
11/5/2019	2	154	198	352	FALSE
11/6/2019	3	177	183	360	FALSE
11/7/2019	4	187	129	316	FALSE
11/8/2019	5	151	135	286	FALSE
11/9/2019	6	4	4	8	FALSE
11/10/2019	_7	3	5	8	FALSE
11/11/2019	_1_	168	171	339	FALSE
11/12/2019	2	184	126	310	FALSE
11/13/2019	3	184	136	320	FALSE
11/14/2019	4	187	111	298	FALSE
11/15/2019	5	178	125	303	FALSE
11/16/2019	6		8	15	FALSE
11/17/2019	7	1 1	9	10	FALSE

11/18/2019	1	156	174	330	FALSE
11/19/2019	2	180	178	358	FALSE
11/20/2019	3	202	222	424	FALSE
11/21/2019	4	163	219	382	FALSE
11/22/2019	5	64	107	171	FALSE
11/23/2019	6	12	7	19	FALSE
11/24/2019	7	1	7	8	FALSE
11/25/2019	1	152	227	379	FALSE
11/26/2019	2	160	220	380	FALSE
11/27/2019	3	113	159	272	FALSE
11/28/2019	4	0	3	3	FALSE
11/29/2019	5	9	15	24	FALSE
11/30/2019	6	4	8	12	FALSE
12/1/2019	7	6	10	16	FALSE
12/2/2019	1	169	212	381	FALSE
12/3/2019	2	237	169	406	FALSE
12/4/2019	3	223	198	421	FALSE
12/5/2019	4	192	156	348	FALSE
12/6/2019	5	152	144	296	FALSE
12/7/2019	6	10	8	18	FALSE
12/8/2019	7	3	7	10	FALSE
12/9/2019	1	168	255	423	FALSE
12/10/2019	2	185	203	388	FALSE
12/11/2019	3	209	195	404	FALSE
12/12/2019	4	191	165	356	FALSE
12/13/2019	5	200	171	371	FALSE
12/14/2019	6	2	6	8	FALSE
12/15/2019	7	1	5	6	FALSE
12/16/2019	1	208	232	440	FALSE
12/17/2019	2	210	248	458	FALSE
12/18/2019	3	227	234	461	TRUE
12/19/2019	4	201	238	439	FALSE
12/20/2019	5	214	236	450	FALSE
12/21/2019	6	7	8	15	FALSE
12/22/2019	7	11	3	14	FALSE
12/23/2019	1	149	139	288	FALSE
12/24/2019	2	15	13	28	FALSE
12/25/2019	3	1	4	5	FALSE
12/26/2019	4	85	132	217	FALSE
12/27/2019	5	83	100	183	FALSE
12/28/2019	6	11	11	22	FALSE
	_		5	8	FALSE
12/29/2019	7	3	[5		171202
12/29/2019		119	144	263	FALSE

Hourly Headcount by Building for Days with Over 120 Unique Badge Swipes

Notes:

The summary below represents the number of personnel ("Headcount") per hour for each building on days which logged over 120 unique Badge Swipes. In total, there are This summary excludes duplicate Employee Badge #s when a unique Employee Badge # swiped into the same building within the same hour. This provides an accurate representation of the number of personnel in a given building for each hour of the day. When referencing the full dataset in Appendix D, the Headquarters Building category represents the "KTM" building. The Motorsports Building represents the Husqvarna and Training buildings.

12/17/2019 - Worst Case Scenario

Date	Hour	Time	HEADQUARTERS	MOTORSPORTS	Total
12/17/2019	0	12:00 AM	0	Ö	0
12/17/2019	1	1 00 AM	0	0	0
12/17/2019	2	2:00 AM	0	0	0
12/17/2019	3	3 00 AM	0	0	0
12/17/2019	4	4:00 AM	0	0	0
12/17/2019	5	5 00 AM	0	0	0
12/17/2019	6	6:00 AM	0	0	0
12/17/2019	7	7:00 AM	0	0	0
12/17/2019	8	8:00 AM	1	1	2
12/17/2019	9	9 00 AM	5	9	14
12/17/2019	10	10:00 AM	25	29	54
12/17/2019	11	11:00 AM	36	32	68
12/17/2019	12	12:00 PM	22	20	42
12/17/2019	13	1.00 PM	10	18	28
12/17/2019	14	2:00 PM	12	17	29
12/17/2019	15	3 00 PM	21	23	44
12/17/2019	16	4:00 PM	29	27	56
12/17/2019	17	5 00 PM	17	25	42
12/17/2019	18	6:00 PM	14	25	39
12/17/2019	19	7 00 PM	7	15	22
12/17/2019	20	8:00 PM	8	5	13
12/17/2019	21	9 00 PM	2	2	4
12/17/2019	22	10:00 PM	1	0	1
12/17/2019	23	11 00 PM	0	0	0

12/18/2019

Date	Hour	Time	HEADQUARTERS	MOTORSPORTS	Total
12/18/2019	0	12:00 AM	0	1	1
12/18/2019	1	1:00 AM	0	0	0
12/18/2019	2	2:00 AM	0	0	0
12/18/2019	3	3:00 AM	0	0	0
12/18/2019	4	4:00 AM	0	1	1
12/18/2019	5	5 00 AM	0	0	0
12/18/2019	6	6:00 AM	0	0	0
12/18/2019	7	7:00 AM	0	0	0
12/18/2019	8	8:00 AM	2	2	4
12/18/2019	9	9:00 AM	9	9	18
12/18/2019	10	10:00 AM	35	29	64
12/18/2019	11	11 00 AM	30	28	58
12/18/2019	12	12:00 PM	22	28	50
12/18/2019	13	1:00 PM	14	19	33
12/18/2019	14	2:00 PM	17	19	36
12/18/2019	15	3:00 PM	19	25	44
12/18/2019	16	4:00 PM	20	23	43
12/18/2019	17	5:00 PM	25	20	45

12/18/2019	18	6:00 PM	15	16	31
12/18/2019	19	7 00 PM	12	8	20
12/18/2019	20	8:00 PM	6	3	9
12/18/2019	21	9:00 PM	0	3	3
12/18/2019	22	10:00 PM	1	0	1
12/18/2019	23	11:00 PM	0	0	0

12/19/2019

Date	Hour	Time	HEADQUARTERS	MOTORSPORTS	Total
12/19/2019	0	12:00 AM	0	1	1
12/19/2019	1	1 00 AM	0	0	0
12/19/2019	2	2:00 AM	0	0	0
12/19/2019	3	3 00 AM	0	0	0
12/19/2019	4	4:00 AM	0	0	0
12/19/2019	5	5 00 AM	0	0	0
12/19/2019	6	6:00 AM	0	0	0
12/19/2019	7	7 00 AM	0	0	0
12/19/2019	8	8:00 AM	1	2	3
12/19/2019	9	9 00 AM	7	9	16
12/19/2019	10	10:00 AM	28	23	51
12/19/2019	11	11:00 AM	32	31	63
12/19/2019	12	12:00 PM	16	19	35
12/19/2019	13	1 00 PM	12	27	39
12/19/2019	14	2:00 PM	15	22	37
12/19/2019	15	3:00 PM	23	21	44
12/19/2019	16	4:00 PM	19	25	44
12/19/2019	17	5:00 PM	21	19	40
12/19/2019	18	6:00 PM	10	22	32
12/19/2019	19	7 00 PM	9	11	20
12/19/2019	20	8:00 PM	8	4	12
12/19/2019	21	9:00 PM	0	2	2
12/19/2019	22	10:00 PM	0	0	0
12/19/2019	23	11:00 PM	0	0	0

12/12/2019

Date	Hour	Time	HEADQUARTERS	MOTORSPORTS	Total
12/12/2019	0	12:00 AM	0	1	1
12/12/2019	1	1:00 AM	0	0	0
12/12/2019	2	2:00 AM	0	0	0
12/12/2019	3	3:00 AM	0	0	0
12/12/2019	4	4:00 AM	0	0	0
12/12/2019	5	5 00 AM	0	0	0
12/12/2019	6	6:00 AM	0	0	0
12/12/2019	7	7 00 AM	0	0	0
12/12/2019	8	8:00 AM	0	3	3
12/12/2019	9	9:00 AM	1	11	12
12/12/2019	10	10:00 AM	8	18	26
12/12/2019	11	11:00 AM	28	22	50
12/12/2019	12	12:00 PM	39	12	51
12/12/2019	13	1:00 PM	17	14	31
12/12/2019	14	2:00 PM	8	15	23
12/12/2019	15	3:00 PM	5	16	21
12/12/2019	16	4:00 PM	12	13	25
12/12/2019	17	5:00 PM	28	10	38
12/12/2019	18	6:00 PM	13	17	30
12/12/2019	19	7:00 PM	13	9	22
12/12/2019	20	8:00 PM	12	4	16

12/12/2019	21	9:00 PM	6	0	6
12/12/2019	22	10:00 PM	1	0	1
12/12/2019	23	11 00 PM	0	0	0

12/11/2019

Date	Hour	Time	HEADQUARTERS	MOTORSPORTS	Total
12/11/2019	0	12:00 AM	0	1	1
12/11/2019	1	1 00 AM	0	0	0
12/11/2019	2	2:00 AM	0	0	0
12/11/2019	3	3 00 AM	0	0	0
12/11/2019	4	4:00 AM	0	0	0
12/11/2019	5	5:00 AM	0	0	0
12/11/2019	6	6:00 AM	0	0	0
12/11/2019	7	7 00 AM	0	1	1
12/11/2019	8	8:00 AM	3	3	6
12/11/2019	g	9 00 AM	8	8	16
12/11/2019	10	10:00 AM	32	20	52
12/11/2019	11	11:00 AM	41	25	66
12/11/2019	12	12:00 PM	13	17	30
12/11/2019	13	1 00 PM	15	16	31
12/11/2019	14	2:00 PM	12	16	28
12/11/2019	15	3 00 PM	22	20	42
12/11/2019	16	4:00 PM	17	20	37
12/11/2019	17	5 00 PM	15	10	25
12/11/2019	18	6:00 PM	11	15	26
12/11/2019	19	7 00 PM	12	8	20
12/11/2019	20	8:00 PM	6	12	18
12/11/2019	21	9 00 PM	1	3	4
12/11/2019	22	10:00 PM	1	0	1
12/11/2019	23	11 00 PM	0	0	0

12/10/2019

Date	Hour	Time	HEADQUARTERS	MOTORSPORTS	Total
12/10/2019	0	12:00 AM	0	0	0
12/10/2019	1	1 00 AM	0	1	1
12/10/2019	2	2:00 AM	0	0	0
12/10/2019	3	3 00 AM	0	0	0
12/10/2019	4	4:00 AM	0	0	0
12/10/2019	5	5:00 AM	0	0	0
12/10/2019	6	6:00 AM	0	0	0
12/10/2019	7	7:00 AM	0	1	1
12/10/2019	8	8:00 AM	2	1	3
12/10/2019	9	9 00 AM	6	14	20
12/10/2019	10	10:00 AM	24	22	46
12/10/2019	11	11:00 AM	37	24	61
12/10/2019	12	12:00 PM	10	13	23
12/10/2019	13	1:00 PM	19	20	39
12/10/2019	14	2:00 PM	16	14	30
12/10/2019	15	3 00 PM	21	16	37
12/10/2019	16	4:00 PM	19	30	49
12/10/2019	17	5:00 PM	8	24	32
12/10/2019	18	6:00 PM	11	17	28
12/10/2019	19	7:00 PM	6	4	10
12/10/2019	20	8:00 PM	3	2	5
12/10/2019	21	9:00 PM	2	0	2
12/10/2019	22	10:00 PM	1	0	1
12/10/2019	23	11.00 PM	0	0	0

12/9/2019

Date	Hour	Time	HEADQUARTERS	MOTORSPORTS	Total
12/9/2019	0	12:00 AM	0	0	0
12/9/2019	1	1 00 AM	0	0	0
12/9/2019	2	2:00 AM	0	0	0
12/9/2019	3	3:00 AM	0	0	0
12/9/2019	4	4:00 AM	0	0	0
12/9/2019	5	5 00 AM	0	0	0
12/9/2019	6	6:00 AM	0	0	0
12/9/2019	7	7:00 AM	0	1	1
12/9/2019	8	8:00 AM	2 3	1	3
12/9/2019	9	9 00 AM	3	5	8
12/9/2019	10	10:00 AM	20	28	48
12/9/2019	11	11:00 AM	32	25	57
12/9/2019	12	12:00 PM	14	23	37
12/9/2019	13	1:00 PM	9	21	30
12/9/2019	14	2:00 PM	10	16	26
12/9/2019	15	3 00 PM	22	30	52
12/9/2019	16	4:00 PM	15	27	42
12/9/2019	17	5 00 PM	13	22	35
12/9/2019	18	6:00 PM	13	26	39
12/9/2019	19	7 00 PM	5	16	21
12/9/2019	20	8:00 PM	9	12	21
12/9/2019	21	9:00 PM	1	1	2
12/9/2019	22	10:00 PM	0	0	0
12/9/2019	23	11 00 PM	0	1	11_

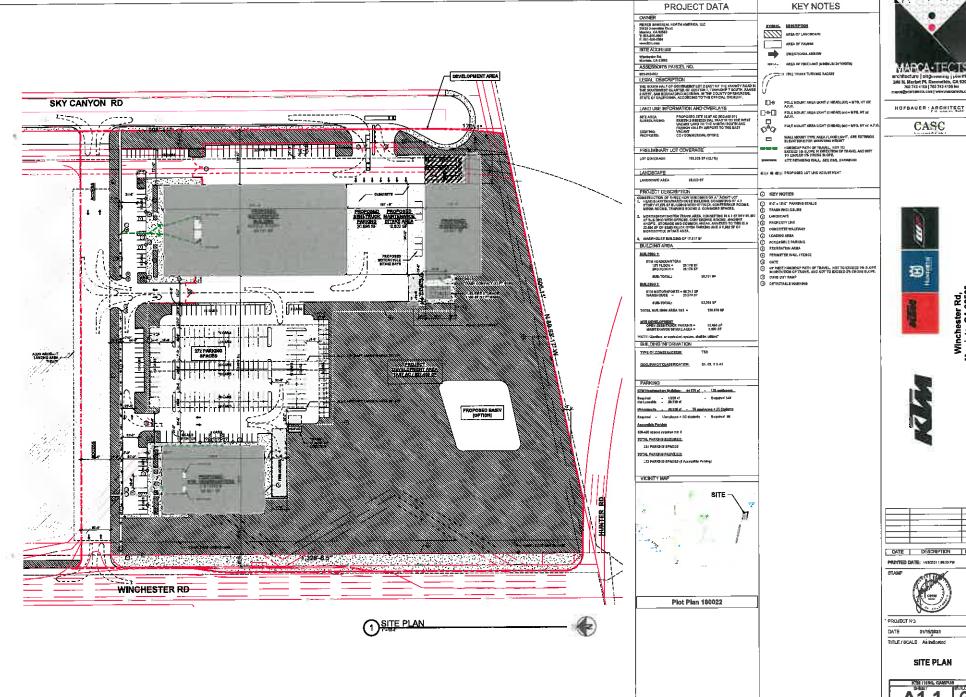
12/4/2019

Date	Hour	Time	HEADQUARTERS	MOTORSPORTS	Total
12/4/2019	0	12:00 AM	0	1	1
12/4/2019	1	1:00 AM	0	0	0
12/4/2019	2	2:00 AM	0	0	0
12/4/2019	3	3:00 AM	0	1	1
12/4/2019	4	4:00 AM	0	0	0
12/4/2019	5	5 00 AM	0	0	0
12/4/2019	6	6:00 AM	0	0	0
12/4/2019	7	7:00 AM	0	1	1
12/4/2019	8	8:00 AM	3	1	4
12/4/2019	9	9:00 AM	12	11	23
12/4/2019	10	10:00 AM	29	21	50
12/4/2019	11	11:00 AM	29	21	50
12/4/2019	12	12:00 PM	12	20	32
12/4/2019	13	1.00 PM	10	11	21
12/4/2019	14	2:00 PM	12	10	22
12/4/2019	15	3:00 PM	23	25	48
12/4/2019	16	4:00 PM	27	21	48
12/4/2019	17	5 00 PM	26	19	45
12/4/2019	18	6:00 PM	19	17	36
12/4/2019	19	7:00 PM	14	8	22
12/4/2019	20	8:00 PM	5	7	12
12/4/2019	21	9:00 PM	1	3	4
12/4/2019	22	10:00 PM	1	0	1
12/4/2019	23	11 00 PM	0	0	0

Date	Hour	Time	HEADQUARTERS	MOTORSPORTS	Total
12/4/2019	0	12:00 AM	0	1	1
12/4/2019	1	1 00 AM	0	0	0
12/4/2019	2	2:00 AM	0	0	0
12/4/2019	3	3:00 AM	0	0	0
12/4/2019	4	4:00 AM	0	1	1
12/4/2019	5	5:00 AM	0	0	0
12/4/2019	6	6:00 AM	0	0	0
12/4/2019	7	7 00 AM	0	0	0
12/4/2019	8	8:00 AM	2	5	7
12/4/2019	9	9:00 AM	6	8	14
12/4/2019	10	10:00 AM	36	22	58
12/4/2019	11	11 00 AM	39	12	51
12/4/2019	12	12:00 PM	14	10	24
12/4/2019	13	1:00 PM	36	19	55
12/4/2019	14	2:00 PM	16	11	27
12/4/2019	15	3 00 PM	18	18	36
12/4/2019	16	4:00 PM	24	22	46
12/4/2019	17	5.00 PM	18	19	37
12/4/2019	18	6:00 PM	15	14	29
12/4/2019	19	7 00 PM	5	6	11
12/4/2019	20	8:00 PM	6	1	7
12/4/2019	21	9 00 PM	2	0	2
12/4/2019	22	10:00 PM	0	0	0
12/4/2019	23	11 00 PM	0	0	0

11/20/2019

Date	Hour	Time	HEADQUARTERS	MOTORSPORTS	Total
11/20/2019	0	12:00 AM	0	1	1
11/20/2019	1	1 00 AM	0	0	0
11/20/2019	2	2:00 AM	0	0	0
11/20/2019	3	3 00 AM	0	0	0
11/20/2019	4	4:00 AM	0	1	1
11/20/2019	5	5 00 AM	0	0	0
11/20/2019	6	6:00 AM	0	0	0
11/20/2019	7	7:00 AM	0	1	1
11/20/2019	8	8:00 AM	0	2	2
11/20/2019	9	9 00 AM	6	9	15
11/20/2019	10	10:00 AM	27	20	47
11/20/2019	11	11 00 AM	22	25	47
11/20/2019	12	12:00 PM	23	22	45
11/20/2019	13	1 00 PM	18	22	40
11/20/2019	14	2:00 PM	15	12	27
11/20/2019	15	3:00 PM	27	20	47
11/20/2019	16	4:00 PM	25	27	52
11/20/2019	17	5.00 PM	14	19	33
11/20/2019	18	6:00 PM	8	21	29
11/20/2019	19	7:00 PM	10	8	18
11/20/2019	20	8:00 PM	6	6	12
11/20/2019	21	9;00 PM	0	5	5
11/20/2019	22	10:00 PM	1	1	2
11/20/2019	23	11:00 PM	0	0	0





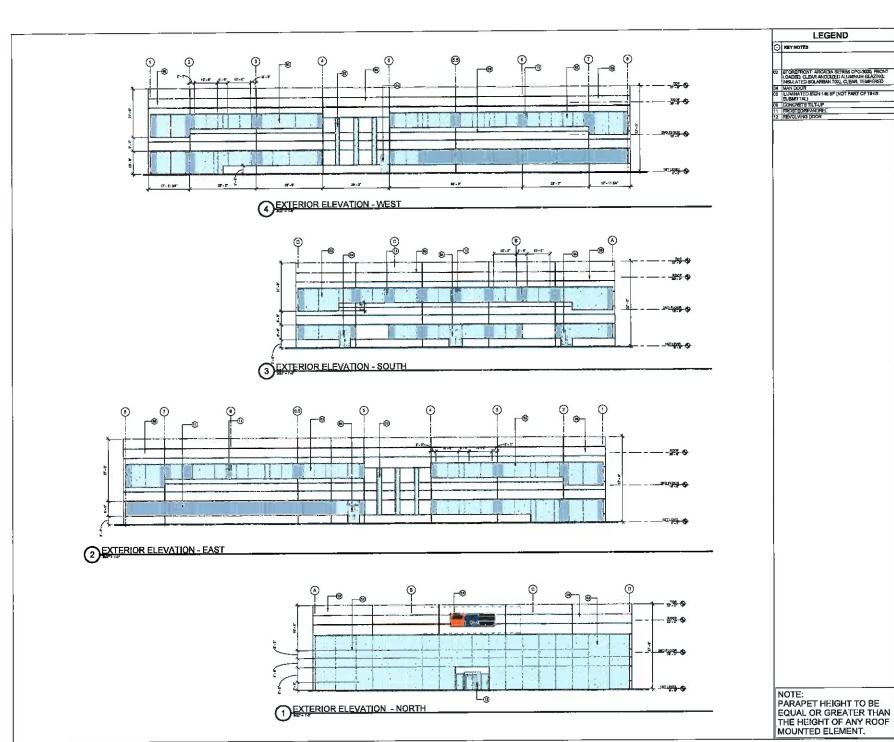
Winchester Rd, Murrieta, CA 92563











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HOFBAUER : ARCHITECT

CASC

3

Winchester Rd, Murrieta, CA 92563



DATE DESCRIPTION NO.

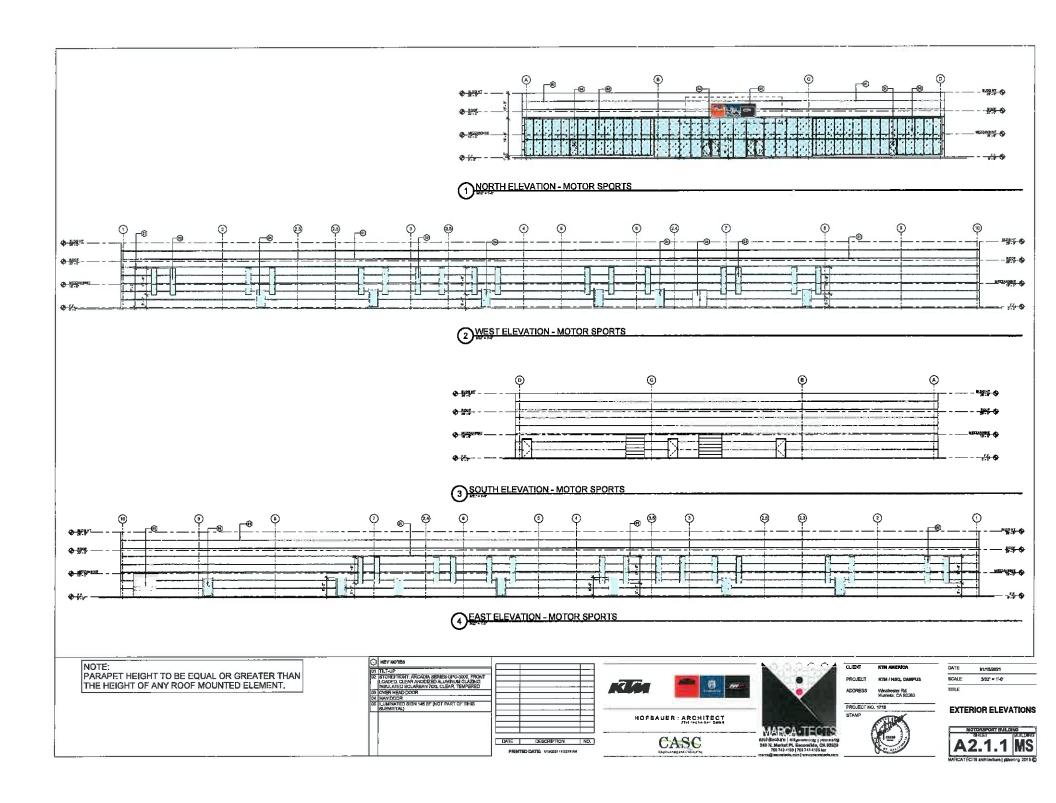
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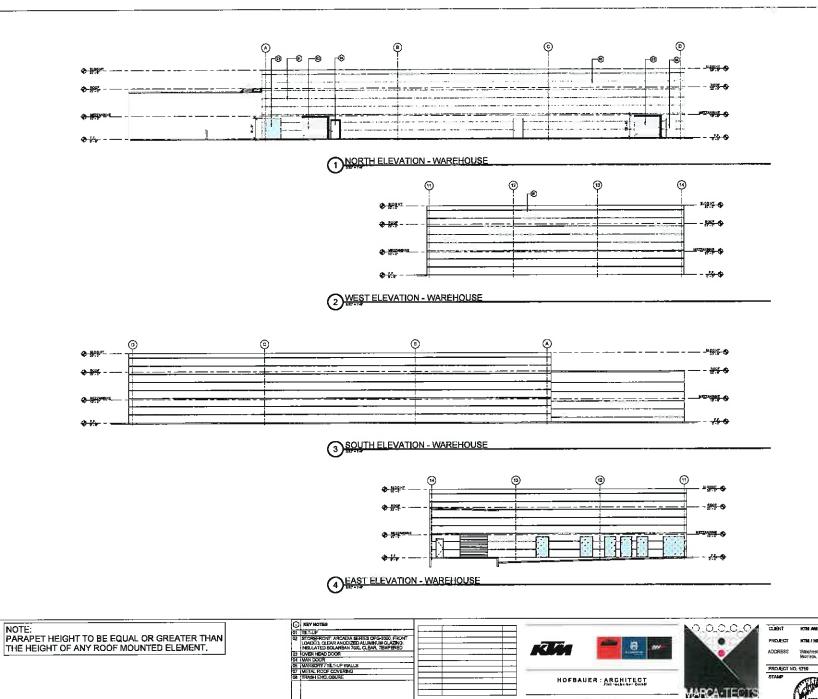


PROJECT NO,

TITLE / 9CALE 3/32" = 1'40"

EXTERIOR ELEVATIONS





DATE DESCRIPTION NO.

CASC

WARENQUE BULDING
STEET
A2.1.2 Wh
MARCATECTS architecture prinring 2016

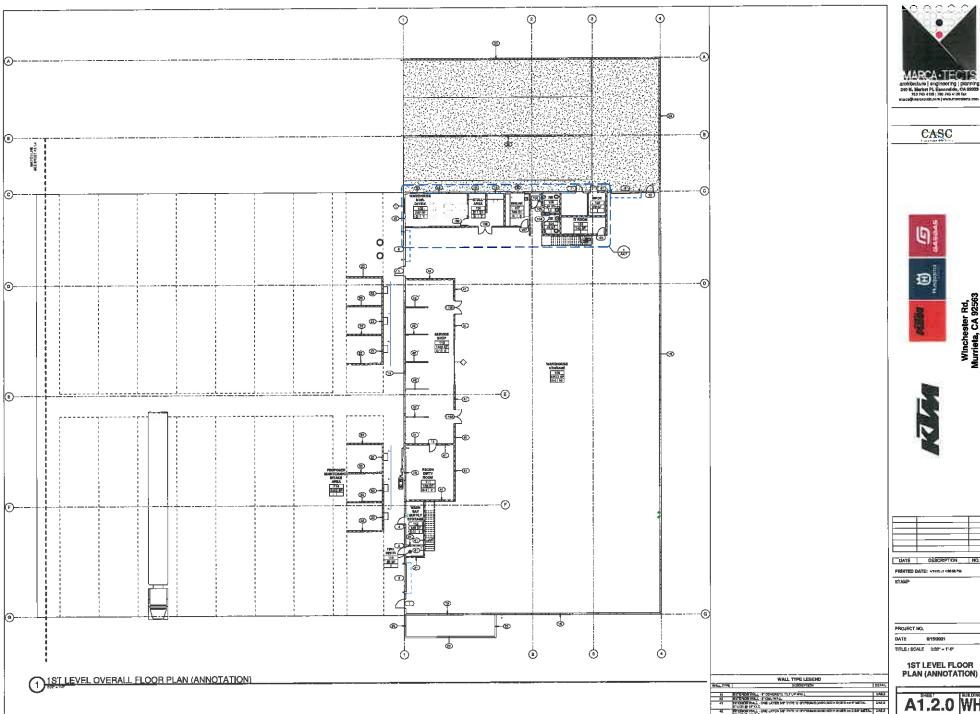
DATE

SCALE

0/15/2021

3/32" = 1'-0"

EXTERIOR ELEVATIONS





MARCATECTS architecture | planning 2016

Winchester Rd, Murrieta, CA 92563





DATE DESCRIPTION NO.

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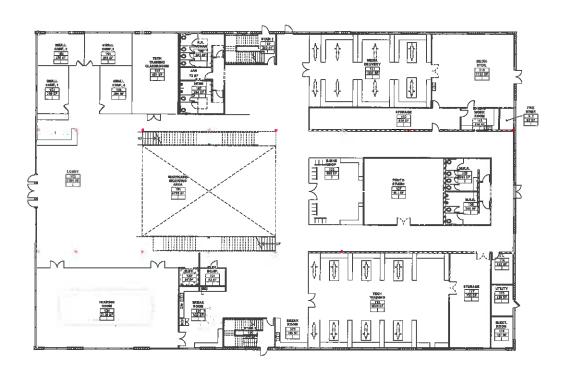


PROJECT NO.

DATE 81/27/2021 TITLE / SCALE 3/32° = 1'-0"

> 1ST LEVEL FLOOR PLAN





1ST LEVEL FLOOR PLAN .









DATE DESCRIPTION NO.

PRINTED DATE: WHATEL SHOULPE

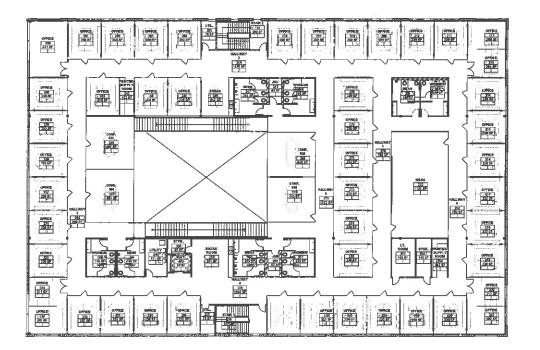


PROJECT NO.

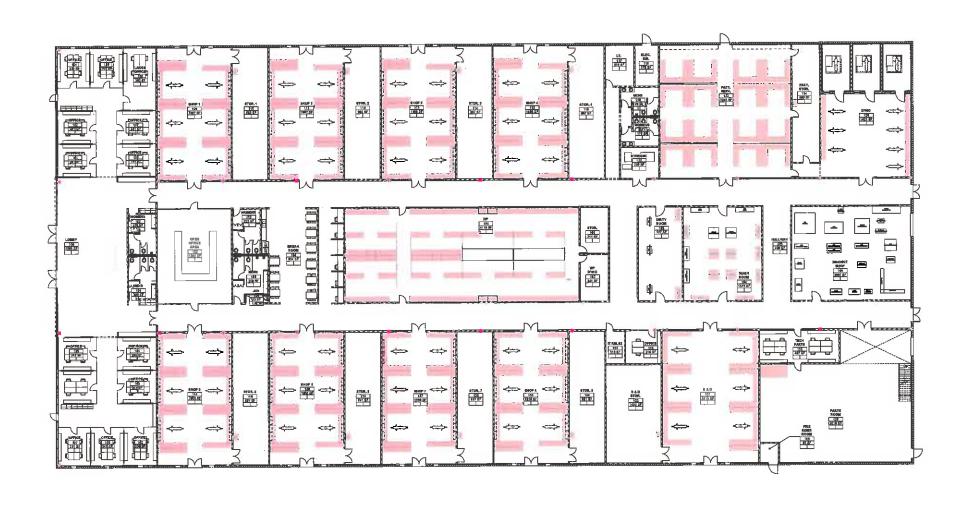
DATE 01/27/2021 TITLE / SCALE 3/32" = 1'-2"

> 2ND LEVEL FLOOR PLAN

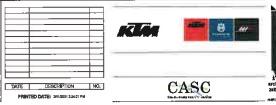
A1.2.3



2ND LEVEL FLOOR



1ST LEVEL FLOOR PLAN M.S. BUILDING





T KTH AMERICA DATE
ECT KITE / Had. CAMPLIS SCALE
ESS Windester Rd.
Murieto. CA 92563

1ST LEVEL FLOOR
PLAN M.S. BUILDING
MOTORUPORT BUILDING
SHEET BUILDING

01/27/2021

3/32" = 1'-0"





1470 E. Cooley Dr. Colton, CA 92324

Phone: (909) 783-0101 ext. 5370 Fax: (909) 783-0108

PROJECT DESCRIPTION

The Project is located within the French Valley community in unincorporated Riverside County on a 31.86 acre parcel identified as APN 963-030-002. The French Valley community consists of residential tract housing, ancillary shops and retail establishments, and a general aviation airport owned and operated by the County of Riverside. The Project is specifically located easterly of Highway 79 (SR-79), otherwise known as Winchester Road and southerly of Sparkman Way. Borel Road borders the Project on a portion of the southerly boundary, and Sky Canyon Drive is located on the easterly boundary. The Project is adjacent to the French Valley Airport and is located within the French Valley Airport Influence Area.

The Project's development area is approximately 19.87 acres, or 852, 469 square feet, on the 31.86 acre parcel. The proposed development will provide 150,789 s.f. commercial office, warehousing, storage, research and development, and covered truck parking with a maintenance intake area on a 19.87 gross-acre site. The largest building will encompass approximately 68,741 square feet (s.f.) which will consist of KTM's motorsport research and development facility. A second building will be approximately 58,351 s.f., which will consist of the KTM headquarters. A third building will consist of a storage warehouse, which will be approximately 23,697 s.f. Additionally, approximately 20,696 s.f. is proposed for an open semi-truck parking area and approximately 8,602 s.f. is proposed for an open maintenance intake area to be adjacent to the Motorsport building. Adjacent to the Warehouse building is approximately 1,300 s.f. for open outdoor fuel/tire storage. Most of the proposed facility will be closed to the public and accommodate the administrative and operational aspects of the business. The development will provide infrastructure and public improvements necessary to support the development and is proposed to begin construction in 4th Quarter of 2021.

KTM HEADQUARTERS BUILDING (58,351 S.F.)

The Headquarters building will consist of general office functions such as sales and marketing, HR, accounting, graphic design and customer service, which are primarily done over phone and internet. Business hours will be Monday through Friday, (8:00am - 5:00pm). This facility will not be open to the public and will be closed to public access due to proprietary information.

MOTORSPORT BUILDING (68,741 S.F.) AND WAREHOUSE (23,697 S.F.) WITH ADJACENT SEMI-TRUCK PARKING (20,696 S.F.) AND MAINTENANCE INTAKE AREA (8,602 S.F.)

The Motorsport building serves as a training and research & development (R&D) facility. Dealer representatives are invited to the facility, where master technicians and mechanics disassemble the bikes by hand and re-build them by hand to show the dealer reps how to repair certain aspects of the bikes, as well as show case certain new functions of the bikes. These are all done for sales and marketing purposes so that the dealers can be experts and have a high level of knowledge over the product to convey to customers. There is no manufacturing or distribution in this facility. There are "Dyno Machines" used to bring the bikes up to speed and test their function.

The warehouse facility will function as a storage for KTM motocross bikes and trucks. The KTM Motorsport company brings in less than 1% of their total manufactured bikes to this facility. In short, the parts are manufactured in Europe, shipped over the Akron, OH where the motocross bikes are assembled. Then



1470 E. Cooley Dr. Colton, CA 92324

Phone: (909) 783-0101 ext. 5370 Fax: (909) 783-0108

they are all placed on trucks, and distributed to dealerships across North America (Canada, Mexico, USA). However, a small percent comes to this facility for testing, marketing, and research and development.

The truck parking area is approximately 20,696 s.f. and anticipates KTM-owned trucks 2-3 times per week during the racing season (3 months out of the year). The maintenance intake area is approximately 8,602 s.f. and will be used to maintain motocross bikes.

ALUC OPEN AREA FOR EMERGENCY LANDING OF SMALL PLANES (2.46 AC)

The Airport Land Use Commission (ALUC) reviews development within airport influence zones to ensure compliance with the Airport Land Use Compatibility Plan. The project site is located within French Valley Airport Influence Zone D and Zone B2. Zone D regulations require the project to dedicate a minimum of ten percent (10%) of the site within its zone as an "open area" for emergency landing of small planes. A total of 24.62 acres of the project site is located within Zone D; thus, the project is required to dedicate a minimum of 2.46 acres of open area within the project site. This area must have a minimum dimension of 75 feet by 300 feet and must be void of any obstacles or structures that are higher than four feet (4') in height or greater than four inches (4") in diameter. The proposed project meets this requirement by dedicating 2.46 acres of open area within the parking lot area and the driveway areas on the site.

NOTICE OF PUBLIC HEARING RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact <u>ALUC Planner Paul Rull at (951) 955-6893</u>. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The Riverside County Planning Department should be contacted on non-ALUC issues. For more information please contact Riverside County Planner Mr. Brett Dawson at (951) 955-0972.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to prull@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please telephone Barbara Santos at (951) 955-5132.

PLACE OF HEARING: Riverside County Administration Center

4080 Lemon Street, 1st Floor Board Chambers

Riverside California

DATE OF HEARING: May 13, 2021

TIME OF HEARING: 9:30 A.M.

Pursuant to Executive Order N-25-20, this meeting will be conducted by teleconference and at the Place of Hearing, as listed above. Public access to the meeting location will be allowed, but limited to comply with the Executive Order. Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org

CASE DESCRIPTION:

ZAP1106FV21 — Pierer Immoreal North America, LLC (Representative: CASC Engineering and Consulting) — County of Riverside Case No. PPT180022S01 (Plot Plan Substantial Conformance), a proposal to construct a KTM motorsport research and development facility including a two-story 58,351 square foot headquarters office building, a 68,741 square foot motorsport research building, and a 23,697 square foot warehouse building (with an outdoor 20,696 square foot semi-truck parking area and an outdoor 8,602 square foot maintenance area) on 19.87 acres located easterly of Winchester Road, westerly of Sky Canyon Road, northerly of Borel Road, and Southerly of Sparkman Way (A proposal to establish a KTM motorsport facility and subsequent tentative parcel map had been previously found consistent by the ALUC) (Airport Compatibility Zones B2 and D of the French Valley Airport Influence Area).



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER	2: PPT180022S01 DATE SUBMITTED: 01/27/2	2021		
APPLICANT / REPRESEN	TATIVE / PROPERTY OWNER CONTACT INFORMATION		-	
Applicant	Pierer Immoreal North America, LLC (c/o Robin Gangruth)	Phone Nu	mber (9	951) 600-8007 x4142
Mailing Address	38429 Innovation Court	Email R	obin.Gran	gruth@ktm.com
	Murrieta, CA 92563			
Representative	CASC Engineering and Consulting (c/o Frank Coyle, Planning Director)	Phone Nu	_	909) 783-0101 x5370
Mailing Address	1470 E. Cooley Drive	Email F	Coyle@ca	ascinc.com
	Colton, CA 92324			
	Disease Improved Mark Associated 11 O (ale 1-b-11)			054) 000 0007 4440
Property Owner	Pierer Immoreal North America, LLC (c/o John Hinz)	Phone Nu		951) 600-8007 x4142
Mailing Address	38429 Innovation Court	Email F	Robin.Grai	ngruth@ktm.com
	Миггieta, CA 92563	•	_	
LOCAL JURISDICTION AC	SENCY	<u> </u>		
Local Agency Name	County of Riverside	Phone Nu	minor (951) 955 1-17
Staff Contact	RRETT DAWS		alvarez@i	
Mailing Address	4080 Lemon Street, 12th Floor		PLOT P	
	P.O. Box 1409	Genera	Plan / Spe	cific Plan Amendment
	Riverside, CA 92502	Zoning Ordinance Amendment Subdivision Parcel Map / Tentative Tract		
Local Agency Project No	PPT180022 5 6 1	Use Per		Map/ Tellacive Hact
		Site Plan Review/Plot Plan Other		
PROJECT LOCATION Attach an accurately scaled is	map showing the relationship of the project site to the airport boundary and runways			
Street Address	37662 & 37862 Winchester Road			
	Murrieta, CA			
Assessor's Parcel No.	963-030-002	Gross Pan	cel Size	19.87
Subdivision Name	N/A		irport and	
Lot Number	N/A	distance f port	FOM AIT-	French Valley - directly adjacent to sit
PROJECT DESCRIPTION If applicable, attach a detaile tional project description date	ed site plan showing ground elevations, the location of structures, open spaces and water bodi ta as needed	es, and the he	eights of stru	ctures and trees; include addi-
Existing Land Use	The Project site is currently vacant. Existing Land Use designation is Commi	ercial Office	e (CO) and	Commercia! Retail (CR)
(describe)	under the Borel Airpark Center Specific Plan. French Valley Airport is d			

Riverside County Airport Land Use Commission, County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, CA 92501, Phone: 951-955-5132 Fax: 951-955-5177 Website: www.rcaluc.org

Proposed Land Use	The Project proposes a mixed-use development of three new buildings. Approximately							
(describe)	58,351 s.f. is proposed for office use; 68,741 s.f is proposed for Motorsport use; and 23,697 s.f for research							
	& development. A	dditionally, 20	0,696 s.f. is proposed	for open semi	-truck parking and 8,60	02 s.f. is maintenance in	take	
For Residential Uses	Number of Parcels o	r Units on Site	e (exclude secondary uni	ts)	N/A			
For Other Land Uses	Hours of Operation Business Hours: 8 a.m 5 p.m. for Headquarte				arters building and R8	D building		
(See Appendix C)	Number of People o	n Site 988				_		
	Method of Calculation Calculation based on Californ			on California I	Building Code (see at	tached spreadsheet)		
Height Data	Site Elevation (above mean sea levei)			1,325		ft.		
	Height of buildings or structures (from the ground)				40		ft.	
Flight Hazards			cteristics which could cr other electrical or visual		oft flight?	Yes No		
	If yes, describe	N/A	N/A					
L								

- A. NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME: Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:
 - 1. Completed ALUC Application Form
 - 1. ALUC fee payment
 - 1. Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
 - 1..... Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
 - 1..... CD with digital files of the plans (pdf)
 - 1..... Vicinity Map (8.5x11)
 - 1. Detailed project description
 - 1..... Local jurisdiction project transmittal
 - 3. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
 - 3. Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. (Only required if the project is scheduled for a public hearing Commission meeting)

COUNTY OF RIVERSIDE AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM: 3.3

HEARING DATE: May 13, 2021

CASE NUMBER: ZAP1457MA21 – Eugene Marini (Representative: Omega

Engineering Consultants)

APPROVING JURISDICTION: City of Riverside

JURISDICTION CASE NO: PR-2021-001023 (General Plan Amendment, Rezone,

Conditional Use Permit, Design Review)

LAND USE PLAN: 2014 March Air Reserve Base/Inland Port Airport Land Use

Compatibility Plan

Airport Influence Area: March Air Reserve Base

Land Use Policy: Zone C2

Noise Levels: Below 60 CNEL contour

MAJOR ISSUES: None

RECOMMENDATION: Staff recommends that the Commission find the proposed General Plan Amendment and Rezone <u>CONSISTENT</u> with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, and find the proposed Conditional Use Permit and Design Review <u>CONSISTENT</u>, subject to the conditions included herein.

PROJECT DESCRIPTION: The applicant proposes to construct a 3,590 square foot drive-thru carwash building on 0.89 acres. The applicant also proposes amending the site's General Plan land use designation from Office (O) to Commercial (C), and changing the site's zoning from Office (O) to Commercial General (CG).

PROJECT LOCATION: The site is located on the northwest corner of Mission Grove Parkway and Alessandro Boulevard within the jurisdiction of the City of Riverside, approximately 17,940 feet northwesterly of the northerly end of Runway 14-32 at March Air Reserve Base.

BACKGROUND:

Non-Residential Average Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zone C2, which limits average intensity to 200 people per acre.

Staff Report Page 2 of 4

Pursuant to Appendix C, Table C-1, of the Riverside County Airport Land Use Compatibility Plan and the Additional Compatibility Policies included in the March ALUCP, the following rates were used to calculate the occupancy for the proposed project:

- Break Room 1 person per 15 square feet
- Storage 1 person per 300 square feet, and
- Office 1 person per 200 square feet.

The project proposes to construct a 3,590 square foot drive-thru car wash, which includes 144 square feet of break room area, 748 square feet of storage area, 156 square feet of office area, and 25 car stacking drive-thru spaces, accommodating a total occupancy of 52 people, resulting in an average intensity of 58 people per acre, which is consistent with the Compatibility Zone C2 criterion of 200.

A second method for determining total occupancy involves multiplying the number of parking spaces provided or required (whichever is greater) by average vehicle occupancy (assumed to be 1.5 persons per vehicle). Based on the number of parking spaces provided (20 parking spaces, 25 car stacking drive-thru space) the total occupancy would be estimated at 68 people for an average intensity of 76 people per acre, which is consistent with the Compatibility Zone C2 criterion of 200.

<u>Non-Residential Single-Acre Intensity</u>: Compatibility Zone C2 limits maximum single-acre intensity to 500 people. There are no risk-reduction design bonuses available, as March Air Reserve Base/Inland Port Airport is primarily utilized by large aircraft weighing more than 12,500 pounds.

Based on the site plan provided and the occupancies as previously noted, the maximum single-acre area would include the entire site, consisting of 144 square feet of break room area, 748 square feet of storage area, and 156 square feet of office area, and 25 car stacking drive-thru spaces, resulting in a single acre occupancy of 52 people, which is consistent with the Compatibility Zone C2 single acre criterion of 500.

<u>Prohibited and Discouraged Uses:</u> The applicant does not propose any uses prohibited or discouraged in Compatibility Zone C2.

<u>Noise:</u> The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site as being below the 60 CNEL range from aircraft noise. Therefore, no special measures are required to mitigate aircraft-generated noise.

Part 77: The elevation of Runway 14-32 at its northerly terminus is 1,535 feet above mean sea level (1,535 feet AMSL). At a distance of approximately 17,940 feet from the project to the nearest point on the runway, Federal Aviation Administration (FAA) review would be required for any structures with top of roof exceeding 1,714 feet AMSL. The site's elevation is 1,604 feet AMSL, and the proposed maximum building height is 29 feet, resulting in a top point elevation of 1,633 feet AMSL. Therefore, review by the FAA OES is not required.

Staff Report Page 3 of 4

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

General Plan Amendment/Rezone: The applicant proposes amending the site's General Plan land use designation from Office (O) to Commercial (C), and changing the site's zoning from Office (O) to Commercial General (CG). The proposed amendments would be consistent with the Compatibility Plan as long as the underlying development's intensity is consistent with the compatibility criteria.

CONDITIONS:

- 1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
- 2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use or activity which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use or activity which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Highly noise-sensitive outdoor nonresidential uses. Examples of noise-sensitive outdoor nonresidential uses that are prohibited include, but are not limited to, major spectator-oriented sports stadiums, amphitheaters, concert halls and drive-in theaters.
 - (f) Hazards to flight.
- 3. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property and be recorded as a deed notice. In the event that the Office

of the Riverside County Assessor-Clerk-Recorder declines to record said notice, the text of the notice shall be included on the Environmental Constraint Sheet (ECS) of the final parcel map, if an ECS is otherwise required.

4. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the stormwater basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the stormwater basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the stormwater basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin

- 5. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
- 6. The project has been evaluated for 3,590 square foot drive-thru car wash, which includes 144 square feet of break room area, 748 square feet of storage area, 156 square feet of office area, and 25 car stacking drive-thru spaces. Any increase in building area, or change in use to any higher intensity use, will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.
- 7. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission and March Air Reserve Base.

NOTICE OF AIRPORT IN VICINITY

airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b)

NOTICE

THERE IS AN AIRPORT NEARBY.

THIS STORM WATER BASIN IS DESIGNED TO HOLD

STORM WATER FOR ONLY 48 HOURS AND

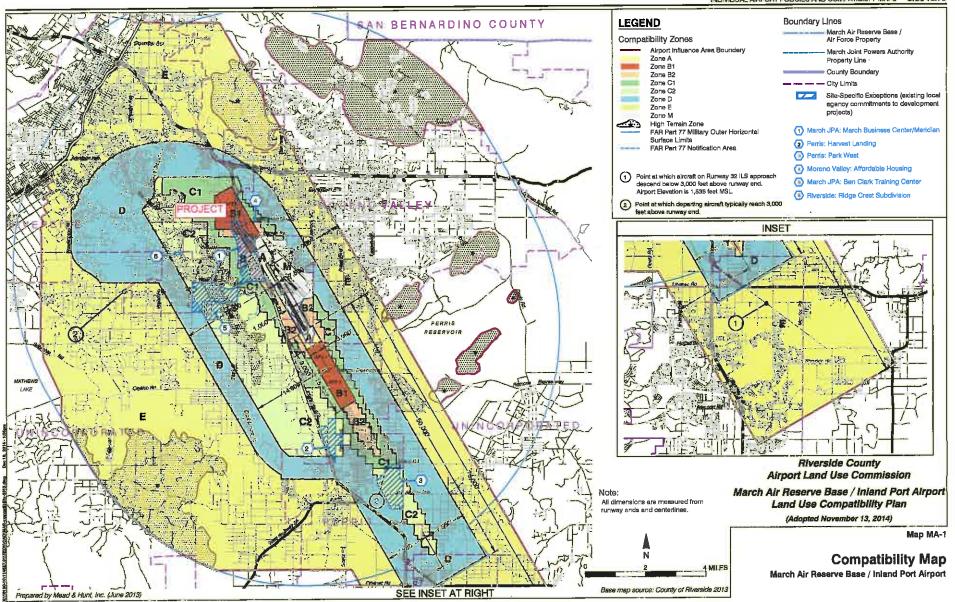
NOT TO ATTRACT BIRDS

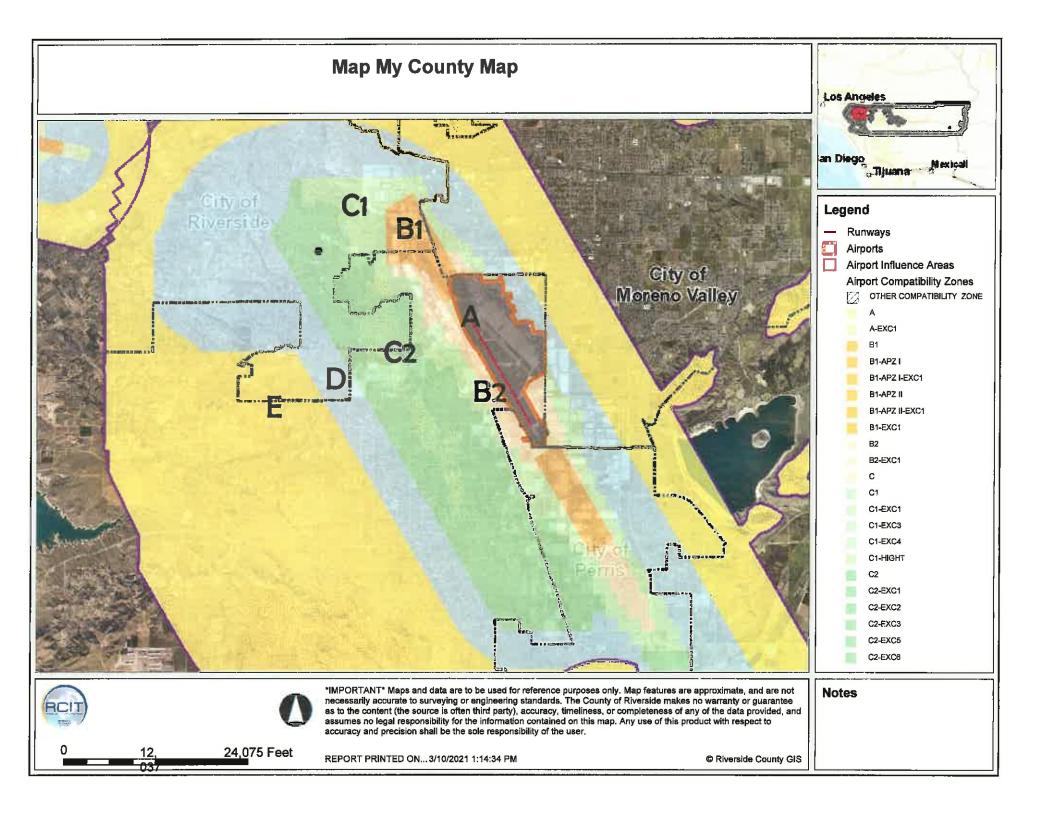
PROPER MAINTENANCE IS NECESSARY TO AVOID BIRD STRIKES

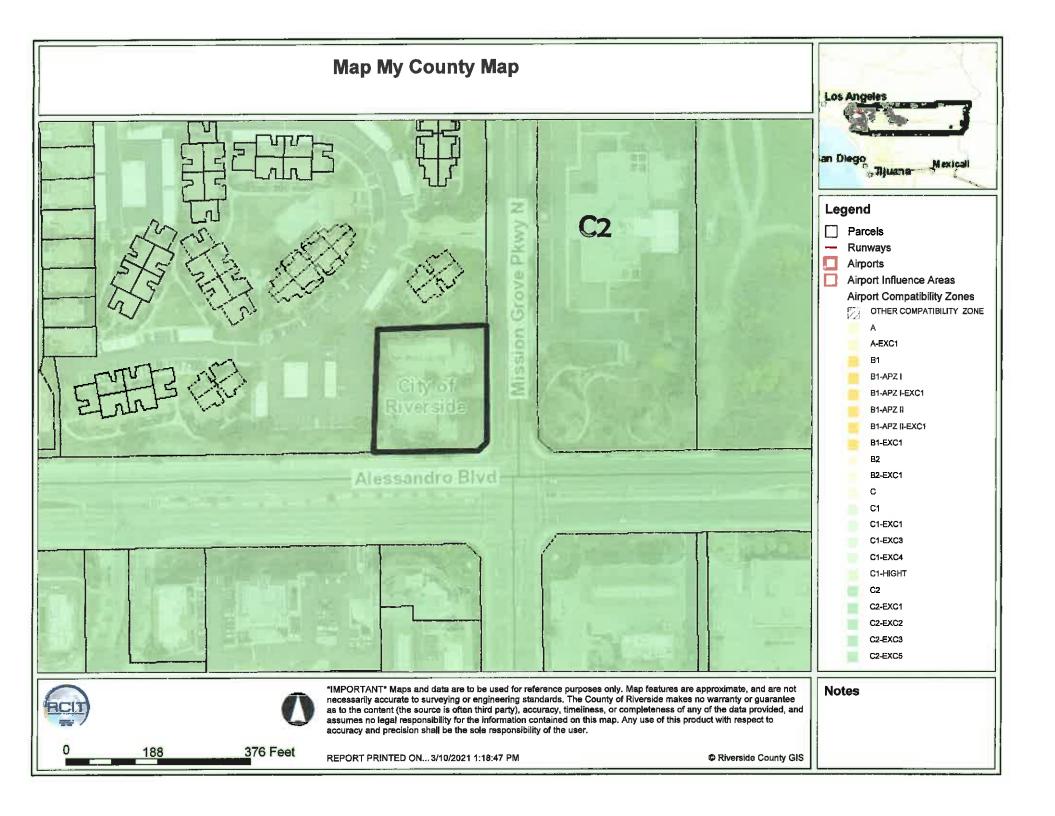


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Name:	Phone:	<u>.</u>	
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Legend

- Parceis
 County Centerline Names
- County Centerlines
 Blueline Streams
- City Areas
 World Street Map





IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

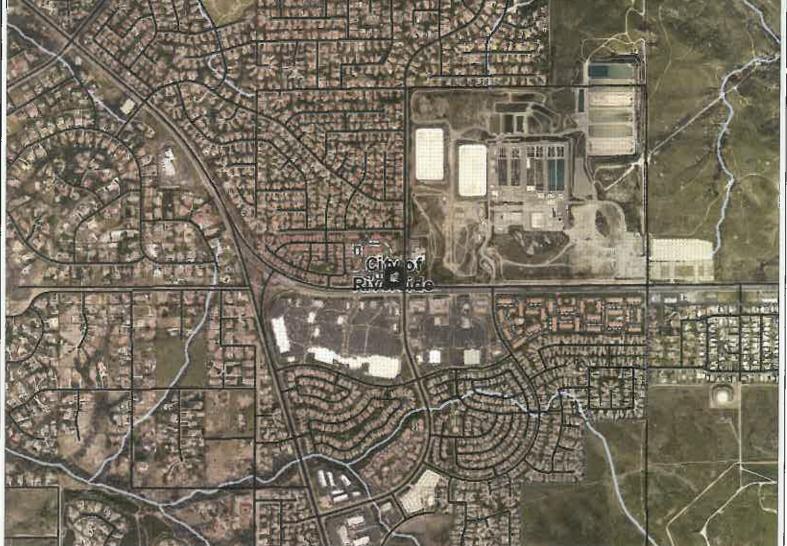
Notes

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376 Feet

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Riverside County GIS





Legend

- County CenterlinesBlueline Streams
- City Areas
 World Street Map





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Notes





Legend

- Parcels
- County Centerlines
 Blueline Streams
 City Areas
 World Street Map





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752 Feet

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Riverside County GIS

Notes





Legend

- Parcels **County Centerline Names**
- **County Centerlines** Blueline Streams City Areas World Street Map





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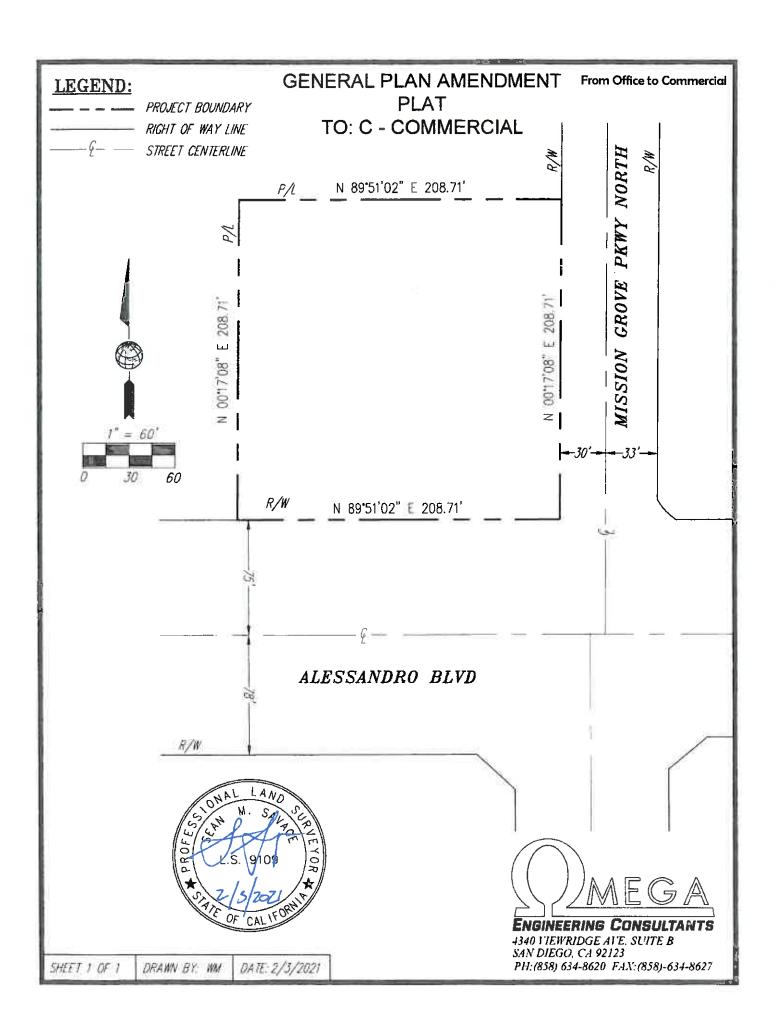
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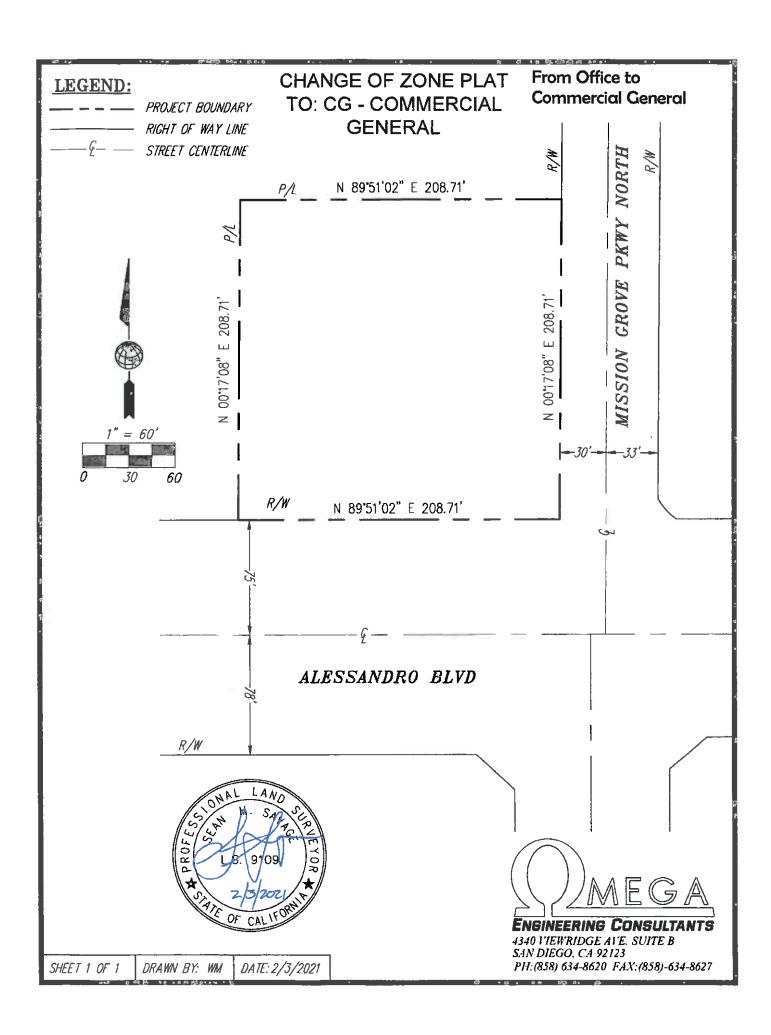
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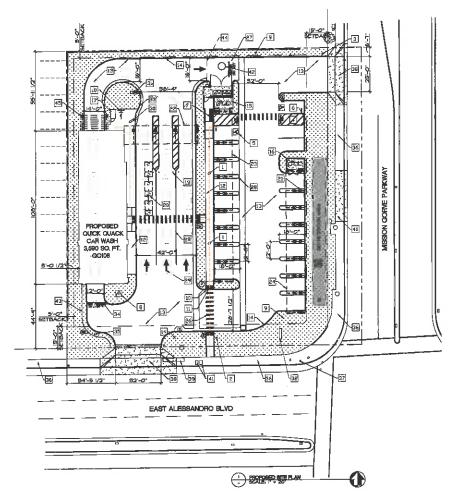
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Notes







PARKING ANALYSIS

TOTAL STALLS

NEM 41-0" X I4"-0" STANDARD STALL

NBM 12'-0" X 18"-0" VACUUM STALLS

MON LONG TERM BIKE PARKING SPACES

NEW 12'-0" X IS-0" ADA STALL

NEN BIKE PARKING SPACES

BITE NUMBER: BUILDING SITE AREA 49.558 STACKING LANES 3 LANE STACIONO WIDTH: 42'-0" VACUUM STALLO: 17 - 12'-0" x 18'-0" VACULAL PRODUCER 2 - 40HP VACULA PRODUCER 2 - 9.20TT
ADA PARRONO! - 9 - 0' × 18'-0'
PARRONO STALLS 2 - 9 × 18'-0'
BULLDING AREA 3.296 5.5".
BULLDING STRINGED NO
OB STATION ATTACHED YES BULDING LENGTH 108'-0" SHOWROOM LENGTH 106'-2"

CONVEYOR TRENCH LENGTH LOC-9 CONVEYOR TYPE REAR MHEEL PUSH

CONVEYOR LENGTH 105'-10'

APPROVAL DATE.

2 STALLS

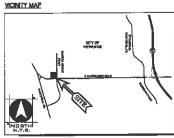
I STALL

11 STA- 1 S

20 STALLS

8 SPACES

SPACES



KEYNOTE9

- 1 ADA PATH OF TRAVEL SHOWN DASHED.
- 2 NEW POLE MOUNTED INTERNATIONAL SYMBOL OF ACCESSIBILITY AT ACCESSIBLE PATH OF TRAVEL.
- 3 NEW TOW AWAY ACCESSIBILITY PARKING SIGN AT EACH ENTRANCE (TYPICAL 2 PLACES)
- 4 NEK VAN ACCESSIBLE VACUM STALL PAINT ACCESSIBLE LOADING ZONE WITH 4" WIDE STRIPING WITH THO COATS OF HIGHMAP BLUE PAVEMENT MARKING PAINT. PAINT THE WORDS "NO PAIKING" IN 12" HIGH LETTERS WITHIN THE LOADING ZONE.
- 5 NEW INTERNATIONAL SYMBOL AT PARKING STALL (TYPICAL 2 PLACES)
- 6 NEW ACCESSIBLE PARKING SIGN. (TYPICAL 2 PLACES)
- 7 NEW TRUNCATED DOMES (TYPICAL 5 PLACES)
- 8 NEW ELECTRICAL TRANSPORMER, VERIFY EXACT LOCATION WITH ELECTRIC COMPANY
- 9 NEW LANDSCAPING SEE LANDSCAPE DRAWINGS BIO-RETENTION AREAS ARE SHADED SEE CIVIL PLANS
- ID NEW FLUSH SURFACE AT TRANSITION (TYPICAL)
- 30 FOOT PLAS POLE, STYLE, "MEMORIAL" UNCOMMON USA, INC, INSTALL PER MANUFACTURER'S INSTALLATION SUIDELINES, FOR REFERENCE ONLY UNDER SEPARATE SUILDING PERMIT
- NEW CONCRETE MALK, MÉDIUM BROOM FINISH FERPENDICULAR TO PATH OF TRAVEL, SLOPE NOT TO EXCEED 3% IN DIRECTION OF TRAVEL, CROSS SLOPE NOT TO EXCEED 2%, SEE CIVIL DRAWINGS AND SOILS REPORT FOR REQUIREMENTS
- MEM CONCRETE DRIVE SLAB HEAVY BROOM FINISH PERPENDICULAR TO TRAFFIC SEE CIVIL DRAWINGS AND SOILS REPORT FOR REGUIREMENTS
- 14 NEW 6" CONCRETE CURB (TYPICAL)
- 15 NEW TRASHAVACIUM ENGLOSURE, SEE DRANING A6.0
- 16 NEW VACUUM EQUIPMENT ENGLOSURE, SEE DRAWING Ab.I
- 17 NEW LONG TERM BIKE LOCKER
- 18 NEW BICYCLE RACK
- 19 NEW 4" CONCRETE FILLED PIPE HOLLARD (TYPICAL & PLACES) (4 AT STACKING LANES AND 2 AT CAR MACH ENTRANCE)
- 20 NEW UNDERGROUND GREASE INTERCEPTOR SEE CIVIL DRAWINGS
- 21 NBN TRASH RECEPTACLES (TYPICAL 16 PLACES)
- EN NEW 131-0" X 551-6" PAY CANOPY .. SEE DRAWING AS.O
- 23 NEW 15'-O" X 120'-2" WEST VACUUM CANOPY SEE DRAMING ASJ
- PM NEW 13'-0" X 100'-2" EAST VACUUM CANOPY SEE DRAWING A5.2
- ES NEW ARTISAN "MOODCRETE" PRÉCAST MALL ENTIRE LENGTH OF PROPERTY LAST 8'-0" SHALL BE 4'-0" TALL
- ES NEW HIGHWAY PAINT WHITE 12" SIDE PEDESTRIAN STRIPING AT 96" O.G. (TYPICAL 3 PLACES)
- ET NEW HIGHWAY PAINT YELLOW 16" "DO NOT ENTER" (TYPICAL 2 PLACES)
- ES NEW HIGHWAY PAINT YELLOW 4" STRIPING
- PRINT HIGHWAY PAINT YELLOW PAVEMENT MARKINGS (TYPICAL)
- 30) NEW DISPLAY/PRICE SIGN REFERENCE ONLY UNDER SEPARATE PERMIT (TYPICAL S PLACES)
- 31 NEW LED SITE LIGHTS (TYPICAL S PLACES) SEE PHOTOMETRIC PLAN
- 32 NEW MONIMENT SIGN REFERENCE ONLY UNDER SEPARATE PERMIT
- 33 24" ROLLED CONCRETE CURB
- 34 NEW TRENCH DRAIN
- 35 NEW "STOP/GO" SIGN
- EXISTING PUBLIC CONCRETE CURB, AND GUTTER TO REMAIN (REPAIR AS REQUIRED BY PUBLIC WORKS)
- 37 EXISTING STOP LIGHT TO REMAIN
- 38 NEW DRIVE APPROACH SEE CIVIL DRAWINGS
- 39 EXISTING BENCH STOP TO BE RELOCATED
- 40 NEW CONCRETE SIDEMALK SEE CIVIL DRAWINGS
- 41 NEW WATER METER SEE CIVIL DRAWINGS
- 42 SUMP PUMP AND CONTROLLER SEE CIVIL DRAWINGS
- 43 96" HIGH GREEN SCREEN
- 44 2 4" PIPE BOLLARDS AND CHAIN FOR EXIT ONLY
- 45 METAL TRELLIS ABOVE ENTRANCE

Architects & Planners, Aut. Make







REVISIONS. O DATE DESCRIPTION

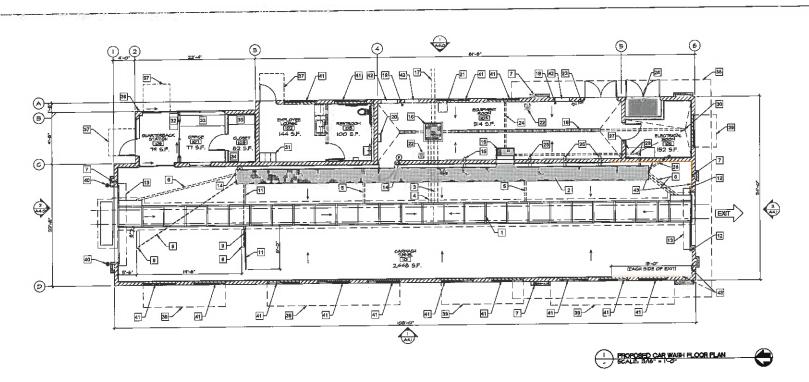
QUICK QUACK CAR WASH STORE 44-202 306 ALESSENDRO BLVD RIVERSIDE, CA. 92506

ARCHITECTURAL SITE PLAN

CRM PROJECT 6



A2.0



KEYNOTES

- 1 CONVEYOR TRENCH, SLOPE AT & PER POOT TO DRAIN.
- 2 UTILITY TRENCH, SLOPE BOTTOM TO DRAIN.
- 3 S' FIPE FROM CONVEYOR TRENCH TO GLEAN OUT PIT.
- 4 6' OVERFLOW PIPE FROM CONVEYOR TRENCH TO CLEAN OUT PIT
- 5 4' FLOOR DRAIN FROM UTILITY TRENCH TO THE CONVEYOR TRENCH SPACED BOUALLY IN UTILITY TRENCH (TYPICAL 2 PLACES)
- 8 4' FIFE CHASE FROM END OF UTILITY TRENCH TO CONVEYOR TRENCH.
- 7 HOSE BIB IN RECESS BOX, PROVIDE WITH LOCKABLE DOOR AT EXTERIOR LOCATIONS, SEE PLINGING GRANINGS (TYPICAL 4 PLACES)
- 8 B/A* DEEP X 6* MIDE X 3*-5* LONG RECESS IN SLAB FOR TIRE SENSOR SMITCH.
 (TYPICAL 2 PLACES)
- (9) 1/2" CONDUIT AT END OF RECESS TO UTILITY TRENCH FOR TIRE SMITCH, CONDUIT TO GO INDER CONVEYOR TRENCH (TYPICAL Z PLACES)
- 10 NOT USED
- $\fbox{11}$ 3/4" x | 1/2" MIDE RECESS IN SLAB FOR DRAINAGE. SLOPE EACH SIDE TO CONVEYOR TRENCH
- 12 3/4" X 2 1/2" MIDE RECRES IN SLAB FOR DRAINAGE, SLOPE EACH SIDE TO CONVEYOR TRENCH, EXTEND "-O" PAST EXIT OPENING
- 13 COILING ROLL-UP DOOR (TYPICAL 2 PLACES)
- 14 MALL MOUNTED ENCLOSED FIRE EXTINGUISHER, 2A-105C (YYPIGAL 2 PLACES)
- 15 9-4" MIDE X 2-2" DEEF OPEN FOR ACCESS TO UTILITY TRENCH.
 CONTRACTOR TO PROVIDE COVER MATCH UTILITY TRENCH
- 18 3'-0" X 5'-0" REGLAM CLEAN-OUT
- 17 8" PIPE FROM GLEAN OUT PIT TO GREASE INTERGEFTOR (GLARIPIERU).
- 18 TRENCH DRAIN.
- 19 ROOF DRAIN LEADER (TYPICAL S PLACES)
- 20 ROOF ACCESS LADDER.
- 21 CONTRACTOR SUPPLIED AND INSTALLED EYE MASH STATION
- 22 16" X 16" X 12" DEEP FLOOR SINK, PLACE SINK AS CLOSE TO MALL AS FOSSIBLE MITHOUT INTERPERIES PUTH FOOTIES (TYPICAL 2 PLACE)

- 23 I" PVG CHASE TO BUS JUICE CONTROL PANEL
- 24 5" CHASE PROM ACCESS OPENING TO PAGE OF MALL
- 25 6" SPECTS FROM UTILITY TRENCH TO EQUIPMENT ROOM (TYPICAL 4 PLACES)
- 26 PLECTRICAL SCRVICE SHITCHSEAR
- 27 2'-0" X 5'-6" X 4" TALL CONCRETE "HOUSE CLEANING PAD" POR MCC FANEL
- 28 3" CHASE NEXT TO CONTROL PANEL TO UTILITY TRENCH)
- 29 Z 8" CHASE AT CENTER OF MCC PANEL "HOUSE KEEPING PAD", RUN ONE CHASE TO EQUIPMENT TRENCH AND ONE NEXT TO ACCESS OPENING
- 38 4" PLOOR DRAIN CONNECT TO TRENCH DRAIN
- 31 EMPLOYEE LOCKERS, MOTAL OF 5 I ADA ACCESSIBLE) CONTRACTOR SUPPLIED AND INSTALLED
- 32 CONTRACTOR FURNISHED AND INSTALLED IS X 56" X 40" TALL POS CABINET
- 33 BAY MAXIMIM A.P.P. COUNTER TOP.
- 34 WALL MOUNTED IT CABINET
- 35 CONTRACTOR PURISHED AND INSTALLED 20" X 54" X 49" TALL STORAGE CARRET HITH ADJISTABLE SHELVING AND PRITE MELANIKE FINSH ON THE INSIDE AND SRAY CUITIDE
- 98 KNOX BOX CONTRACTOR TO COORDINATE MITH LOCAL PIRE DEPARTMENT FOR TYPE, MIZE AND PREFERRED LOCATION
- 37 LEVEL LANDING POR CBC IIB-4042,4 AND TABLE IIB4042,4.) (TYP) 24" MIN STRIKE EDGE CLIPARANCE FOR THE EXTENDED DOOR LANDING
- THE OF ROOF ABOVE.
- 38 WALL MOUNTED CANOPY ABOVE (TYPICAL 5 FLACES)
- 4" GONCRETE FILLED FIFE BOLLARD AT ENTRANCE, (TYPICAL 2 PLACES)
- 41 PAUX HINDOM SEE EXTERIOR ELEVATIONS
- 42 93" MOE X IS" HIGH AIR VENT SEE EXTERIOR ELEVATIONS (TYP. 9 PLACES)
- 43 ACQUETIBLOK "GUIET FIBER" PANELS

NOTE: ALL CONDUITS TO HAVE SHEEPS ONLY, 90 DESIREE CONDUIT/CHASE BENDS ARE NOT ALLOWED. (rchitects & Planners, Inc.







REVISIONS DATE DESCRIPTION

CUICK OUACK CAR WASH STORE 14-202 \$06 ALESSIENDRO BLVD HIVERSIDE, CA. 92508

FLOOR PLAN

5 JANGARY



A3.0

OWNER

KANNON AGHARU, MICHEL OF DE CANTUL TRINACIA CACUP. NO DETRED RENENT POISON MICHE KANDEN A. ACS, MICHEL OF DE FLAKES BEREINHARUN, MC DETRED RENENT POISON MICHE KANDEL ACHARU, MICHEL OF THE KEY MC DETRED RENENT FRANKR MICH.

LESSEE:

SITE ADDRESS:

OUTCH COMON CAR MILES 1,380 1EAD HOLL BLND (2018 HOSEMULE CAL 95881

N CAR MUST JEB EAST MESSANDARD BORREVAND. MEL BLYD 2200 MINERSINE, CA 92000 NJ. 02007

ASSESSOR'S PARCEL NUMBER:

212-860-004-

TITLE INFORMATION:

THE INFORMATION FOR THE SIRVEY BASED ON A PREEMBARY REPORT PREPARED BY PROEUTY MADDING. THE COMPARY AS ORDER NO. 997-3000 TO -0-MIS, AMERICANOV NO. 8 DATES AUGUST 20, 2015.

LEGAL DESCRIPTION:

THE LAND REPORTED TO HEREM DELOW IS STOMED AS HE CITY OF INVOCACE IN THE COUNTY OF REPORTED, STATE OF CHAPOOLS, AND IS DESCRIBED AS FOLLOWS:

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EXCEPT BUST PRIMER L'UNG SOURCEUT OF A LINE 40 FEET MARILL OF AND PRIVALLE, MEN THE SOURL UNE OF SCISSIE & THANSAIR J. SOURL, RIMEZ & NEST, SAM EXEMPLIAND MERSON, AS DESCRIED IN DOES TO THE COUNTY OF MICHIESE SECURICAL FAME 76, 1958 IN ADDR. 1591, PAIZ 500 OF OFFICIAL RECURREN.

VEHTICAL BENCHMARK:

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ELEWIDON HON HOS (NAVOS

SOURCE OF TOPOGRAPHY:

FEED SURVEY COMPLETED BY CHECK LAND SURVEYING, MIC ON MAY IS, 2020.

BASIS OF BEARINGS:

THE BASS OF TEATHORS FOR THE SURFEY IS THE CONTRIBUTE OF MISSION CROSS PARKWAY MORPH AS SHOWN ON HIS 187/18-17, SAG BECAUSE SCHOOL TO CATTON E

AREA SUMMARY:

TOTAL PARCEL AREA (NET) JEANS

TOTAL PARCEL AREA (GROSS) 4

MESTHOUS (PROPOSED)

X METHODS (DISTRIC)

EASEMENTS:

(I) MONT-OF-MAY EXEMBET COMPRES TO THE OTY OF MARKED FOR PARKET STREET AND MARKEN PROPERTY STREET COMPRES TO THE TABLE STREET AND MARKET PROPERTY PROPERTY STREET AND MAY 2, 1987, AS DOCUMENT MD, 130302



SEAN IL SILVAGE R.C.E. 75877

SATE

EXISTING LEGEND

IBM SMOT PROPERTY LIKE CONSTRUKE IRCH-GE-MIT SK. KERMANG MILL

PROPOSED LEGEND

STANKE THE R FT-978.00 PROPOSED PARSH ROOM ELEVATOR. .574.007C PROPOSED PARISHENT REPORTED 374.000 PROPOSED PYT, STORE GRAN (SZE PTR PLAN) PROPOSED PVI. BOOFDRAIS. PROPOSED PAT. SERRE LINE (SEE PAR FLAN)......

THE COMMERCE WAS .

VI. TOC PARENT HARRIS.

VI. TOC PARENT HARRIS.

VI. T. C. PARENT HARRIS.

VI. T. C. PARENT HARRIS.

VI. T. C. PARENT HARRIS.

VI. LABOZIANIA.

VI. DARZ SHADAI.

PROPOSED OFFSTE CURE OURLET.

PROPERTO OFFICE FOC PARTIES.

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REVISIONS

• DATE DESCRIPTION

QUICK QUACK CAR WASH BTORE M4-202 306 ALESSENDRO BLYD RVERSDE, CA. 92008

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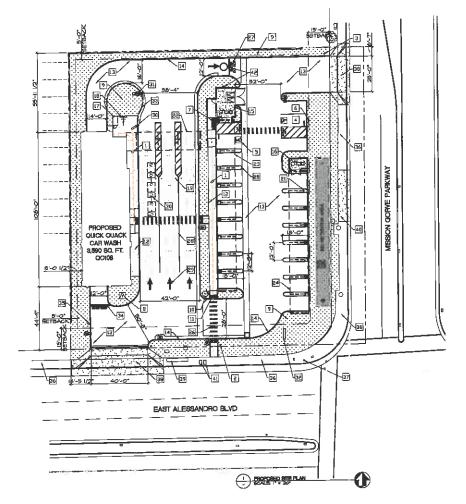
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LILDING	QQ-108

berreta.	
SITE AREA: 43.5	58
STACKING LANE	B 3 LANES
STACKING WIDTH	42-0
VACUUM STALLS	17 - 12'-0" x 18'-0"
VACUUM PRODU	OFR 2 - 40HP
ADA BARKBEN	9'-0" x 18'-0"
PLANTAGE STALL	2 - 9 X 18 -0"
BULDING AREA	3500 S.F
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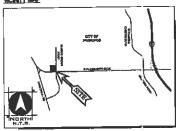
CONVEYOR TYPO REAR MHEEL PUSH

APPROVAL DATIB

PARKING ANALYSIS

NEW 4'-0" X MI-O" STANDARD STALL	2 STALLS
NBH 12:-0" X IS-0" ADA STALL	I STALL
NON 12'-0" X IB'-0" VACUUM STALLS	IT STALLS
TOTAL STALLS	20 STALLS
NON BIKE PARKING SPACES	3 SPACES
NEW LONG TERM BIKE PARKING SPACES	I SPACES

VICINITY MAP



KEYNOTEB

- I ADA PATH OF TRAVEL SHOWN DASHED.
- 2 NEW FOLE MOUNTED INTERNATIONAL SYMBOL OF ACCESSIBILITY AT ACCESSIBLE PATH OF TRAVEL.
- 3 NEW TOW AWAY ACCESSIBILITY PARKING SIGN AT EACH ENTRANCE (TYPICAL 2 PLACES)
- [4] NEW VAN ACCESSIBLE VACUUM STALL PAINT ACCESSIBLE LOADING ZORE MITH 4" MIDE STRIPING MITH THO COATS OF HIGHNAY BLUE PAVENENT MARKING PAINT, PAINT THE WORDS NO PARKING" IN 12" HIGH LETTERS WITHIN THE LOADING ZONE.
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- NEW CONCRETE WALK, MEDIUM BROOM FINISH PERFENDICULAR TO PATH OF TRAVEL, SLOPE NOT TO EXCEED 5% IN DIRECTION OF TRAVEL, CROSS SLOPE NOT TO EXCEED 5% SEE CIVIL DRAWNINGS AND 2011S REPORT FOR REQUIREMENTS
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- [9] NEW 4" CONCRETE FILLED PIPE BOLLARD (TYPICAL & PLACES) (4 AT STACKING LANES AND 2 AT CAR MASH ENTRANCE)
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- 22 NEM 18'-0" X 53'-5" PAY CANOPY SEE DRAWING AS.O
- 83 NEW 18'-0" X 120"-2" WEST VACUUM CANOPY SEE DRAWING A5.1
- 24 NEW 13'-0" X 100'-2" EAST VACUUM CANOPY SEE DRAWING A5.2
- ES NEW ARTISAN WOODCRETE" PRECAST WALL ENTIRE LENGTH OF PROPERTY LAST 8'-O" SHALL BE 4'-O" TALL
- ES NEW HIGHWAY PAINT WHITE (2" SIDE PEDESTRIAN STRIPING AT 36" O.C. (TYPICAL 3 PLACES)
- PAN HIGHMAY PAINT YELLOW 18" "DO NOT ENTER" (TYPICAL 2 PLACES)
- 28 NEW HIGHMAY PAINT YELLOW 4" STRIPING
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- 37 EXISTING STOP LIGHT TO REMAIN
- 38 NEW DRIVE APPROACH SEE CIVIL DRAWINGS
- 39 EXISTING BENCH STOP TO REMAIN
- 40 NEW CONCRETE SIDEMALK SEE CIVIL DRAWINGS
- 41 NEW WATER METER SEE CIVIL DRAWINGS
- AND WIND DIME AND CONTROLLED ARE CIVIL DEAWNER







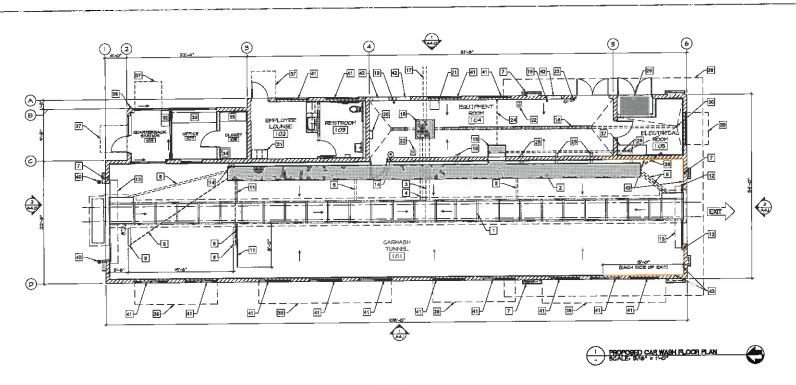
REVISIONS O DATE DESCRIPTION

QUICK QUACK CAR WASH STORE 14-202 306 ALESSENDRO BLVD RIVERSIDE, CA. 92508

ARCHITECTURAL. SITE PLAN



A2.0



KEYNOTES

1 CONVEYOR TRENCH, SLOPE AT ! PER POOT TO DRAIN.

2 UTILITY TRENCH, SLOPE BOTTOM TO DRAIN,

3 8" PIPE FROM CONVEYOR TRENCH TO CLEAN OUT PIT.

4 6" OVERFLOW PIPE FROM CONVEYOR TRENCH TO CLEAN OUT PIT

5 4' FLOOR DRAIN FROM UTILITY TRENCH TO THE CONVEYOR TRENCH SPACED EQUALLY IN UTILITY TRENCH (TYPICAL 2 PLACES)

 $\fbox{6}$ 4' PIPE CHASE FROM BND OF UTILITY TRENCH TO CONVEYOR TRENCH, (TYPICAL 3 PLACES)

7 HOSE BIB IN RECESS BOX, PROVIDE WITH LOCKABLE DOOR AT EXTERIOR LOCATIONS, SEE FLIMBING DRAWINGS (TYPICAL 4 PLACES)

8 3/4" DEEP X 6" WIDE X 3"-5" LONG RECESS IN SLAB FOR TIRE SOSOR SMITCH, (TYPICAL 2 SLACES)

9 1/2" CONDUIT AT END OF RECESS TO UTILITY TRENCH FOR TIRE SMITCH, CONDUIT TO GO UNDER CONVEYOR TRENCH (TYPICAL 2 FLACES)

10 NOT 1950

11 9/4" X I 1/2" MIDE RECESS IN SLAB FOR DRAINAGE, SLOPE EACH SIDE TO CONVEYOR TRENCH

12 3/4" X 2 1/2" MIDE RECESS IN SLAS FOR DRAINASE, SLOPE EACH SIDE TO CONVEYOR TRENCH, EXTEND 1"-0" PAST EXIT OPENING

13 COILING ROLL-UP DOOR, (TYPICAL 2 PLACES)

14 MALL MOUNTED ENCLOSED FIRE EXTINGUISHER - 2A-IOBG (TYPICAL 2 PLACES)

15 3'-4' YIDE X 2'-2' DEEP OPEN FOR ACCESS TO UTILITY TRENCH.
CONTRACTOR TO PROVIDE COVER - MATCH UTILITY TRENCH.

16 3'-0" X 9'-0" RECLAIM GLEAN-OUT

17 8" PIPE FROM GLEAN OUT PIT TO GREASE INTERCEPTOR (CLARIFIET)

19 ROOF DRAIN LEADER (TYPICAL 3 FLACES)

20 ROOF ACCESS LADDER.

[21] CONTRACTOR SUPPLIED AND INSTALLED BYE MASH STATION

22 16" X 16" X 12" DEEP FLOOR SINK, PLACE SINK AS CLOSE TO MALL AS FOSSIBLE NITHOUT INTERPERING MITH FOOTING (TYPICAL 2 PLACES)

23 IF PVG CHASE TO BUS JUICE CONTROL FANEL

24 3" CHASE PROM AGGESS OFFINING TO FACE OF MALL

6" SMEETS FROM UTILITY TRENCH TO EQUIPMENT ROOM (TYPICAL 4 FLACES)

28 ELECTRICAL SERVICE SMITCHGEAR

27 2'-0" X 5'-6" X 4" TALL CONCRETE THOUSE CLEANING PAO" POR MCG PANEL

28 S' CHASE (NEXT TO CONTROL PANEL TO UTILITY TRENCH)

 2° 2 - 8" chase at center of MCC parel "house keeping pao", rin one chase to egyphent trench and one next to access opphing

30 4" PLOOR DRAIN - CONNECT TO TRENCH PRAIN

31) EMPLOYEE LOCKERS, (TOTAL OF 3 - I ADA ACCESSIBLE)
CONTRACTOR SUPPLIED AND INSTALLED

32 CONTRACTOR FURNISHED AND INSTALLED IS X 96" X 40" TALL FOS CABINET

33 54" MAXIMUM A.F.F. COUNTER TOP

37 LEVEL LANDING FER CBC IB-404.2.4 AND TABLE IB404.2.4.1 (TYF) - 24" MIN STRIKE EDGE CLEARANCE FOR THE EXTERIOR DOOR LANDING

38 LINE OF ROOF ABOVE.

39 HALL MOUNTED CANOPY ABOVE, (TYPICAL 5 FLACES)

4" CONCRETE FILLED FIFE BOLLARD AT ENTRANCE (TYPICAL 2 PLACES)

41 FAUX MINDON - SEE EXTERIOR ELEVATIONS

42 32' MIDE X 16" HIGH AIR VENT - SEE EXTERIOR ELEVATIONS (TYP. 5 PLACES)

43 AGOUSTIBLOK "GWET FIBER" PANELS

NOTE: ALL CONDUITS TO HAVE SMEEPS ONLY, NO DESPREE CONDUIT/CHASE BENOS ARE NOT ALLONDO

& Planners,







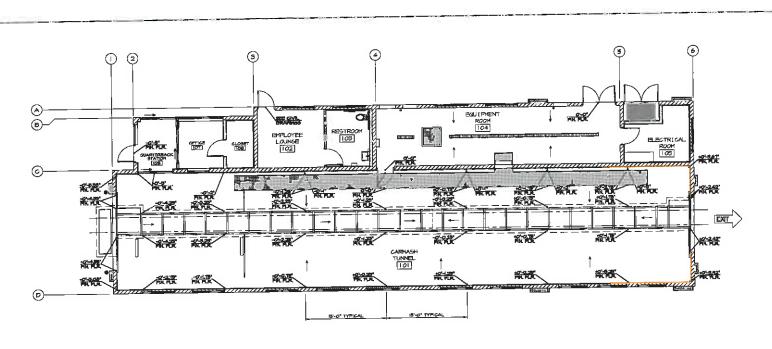
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FLOOR PLAN



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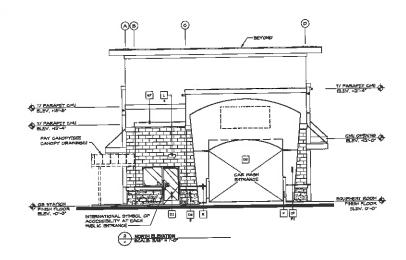
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- PLASTER COLOR COAT TO BE SAND FINISH
- DI HOLLOW METAL DOOR PAINT TO TO MATCH SHERPIN MILLIAMS \$7025 BACKDROP
- CYBRICAD METAL DOOR PAINT TO TO MATCH SHERING MILLIAMS #1025 BACKDROP
- CLEAR ANDDIZED ALIMBUM COMMERCIAL GRADE STORESTRONT GLASS DOOR
- 8" X 16" THROUGH MALL SCUPPER, COLLECTOR AND DOWNSPOUT, PAINT TO MATCH ADJACENT SURFACE
- EJ 1/4' DEET REVEAL EXTANSION JOINT
- - LI = LEO RADIAL MALL SCONCE (DOWN ONLY) L2 = LINEAR PACADE MALL LIGHT (DOWN ONLY) L6 = WALL PACK (DOWN ONLY)

CONTACT STEVE PRIEDMAN (\$47.830.1444 NITH HERNITAGE LIGHTING FOR ORDERING LIGHTING, (NO EXCEPTIONS)

- METAL FAGGIA PAINT TO MATCH SHERVIN
- 4" PIPE BOLLARD HILLED WITH CONCRETE WITH PROTECTIVE SLEEVE
- 2' MIDE X V4' DEEP CLEAR ANODIZED ALIMINIM PLASTER REVEAL
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- 5 SIGNAGE BY OTHERS SHOWN DASKED- FOR REFERENCE ORLY UNDER SEPARATE PERMIT, GENERAL CONTRACTOR TO GOODDINATE WITH SIGN COMPANY.
- STANDING SEAM METAL ROOP PIRESTONE UCB "DARK IVY".
- METAL MALL CANOPY SYSTEM COLOR TO MATCH SHERWIN MILLIAMS "GUICK GLACK GREEN"
- MALL CANCPY TIE-BACK SYSTEM COLOR TO MATCH SHERVIN WILLIAMS "QUICK QUACK GREEN"
- 52" MIDE X 16" HIGH VENT PAINT TO MATCH ADJACENT FINSH
- EXTERIOR MINDOW ASSEMBLY TEMPERED SLASS CLEAR ANDRIGED ALIMINIM STOREPRONT
- EXTERIOR FAIX MINDOM ASSEMBLY MITH BLACK
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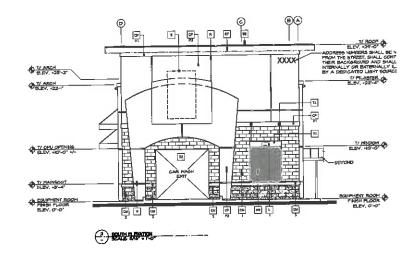
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EXTERIOR **ELEVATIONS**





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KEY NOTES

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- HOLLOW METAL DOOR PAINT TO TO MATCH SHERMIN MILLIAMS #1025 "BACKDROP"
- OVERNEAD METAL DOOR PAINT TO TO MATCH SHERVIN
- 01 CLEAR ANDDIZED ALIMINIM COMMERCIAL SRADE STORESTONE GLASS DOOR
- 8" X 16" THROUGH MALL SCUPPER, COLLECTOR AND DOWNSPOUT, PAINT TO MATCH ADJACENT SURFACE
- EI V4" DEEF NEVEAL EXPANSION JOINT
- LIGHT FIXTURE
 - LI : LED RADIAL MALL SCONCE (DOMN ONLY) L2 : LINEAR MACADE WALL LIGHT (DOWN ONLY) L3 : 2 ROOT LED PILASTER (LIGHT (DOWN ONLY) L4 : MALL PACK (DOWN ONLY)
 - CONTACT STEVE PRIEDMAN 1,841,830,1444 PITH HERMITAGE LIGHTING FOR ORDERING LIGHTING, INC
- METAL FASCIA PAINT TO MATCH SHERMIN MILLIAMS "GUICK GLACK GREEN" (GREEN)
- 4" MIPE BOLLARD FILLED MITH CONCRETE MITH PROTECTIVE SLEEVE

- 83 DIA PVC ROOF OVERFLOW SCUTTER PAINT TO MATCH ADJACENT PINISH
- SIGNASE BY OTHERS SHOWN DASHED- FOR POTERING ORLY INDEX SEPARATE FERMIT, SCHERAL CONTRACTOR TO COORDINATE WITH SIGN COMPANY.
- STANDING SEAM METAL ROOF FIRESTONE UCS "DARK IVY".
- METAL MALL CANOPY SYSTEM COLOR TO MATCH SHERMIN MILLIAMS "QUICK QUACK GREEN"
- 92" MIDE X IS" HIGH VENT PART TO MATCH ADJACENT FINISH
- EXTERIOR WINDOW ASSOCIATE TEMPERED GLASS CLEAR ANODIZED ALIMINIM STOREFRONT
- WZ EXTERIOR FAIX MINDON ASSEMBLY WITH BLACK CERAMIC PRIT TO-PERED SPANDED, SLASS CLEAR ANODIZED ALIMINIM STOREFRONT

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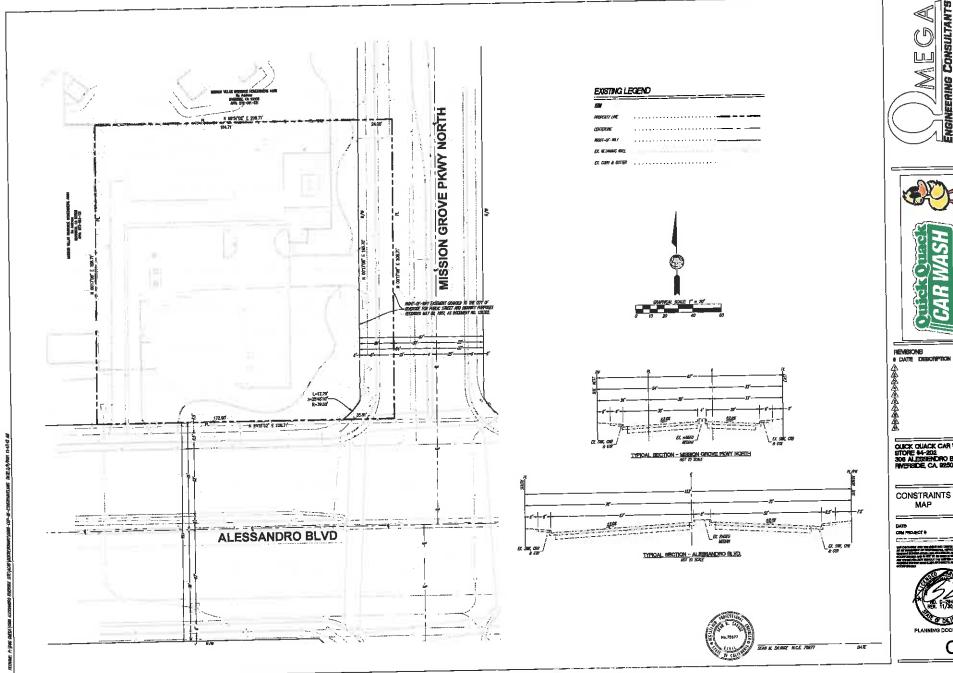
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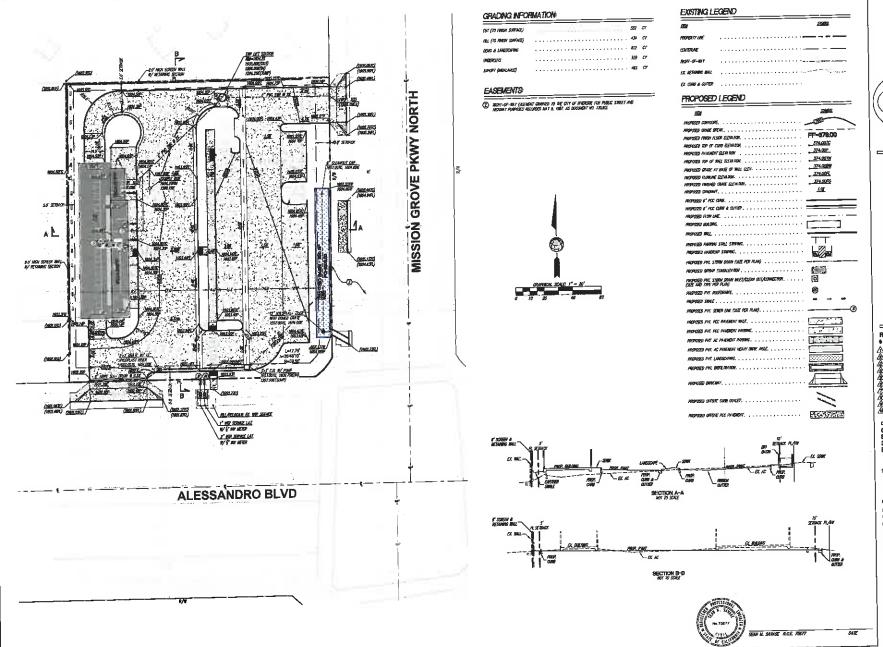


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CONSTRAINTS MAP



C1.0



ENGINEERING CONSULTANTS
SAAVEWLOGG, CA 92123



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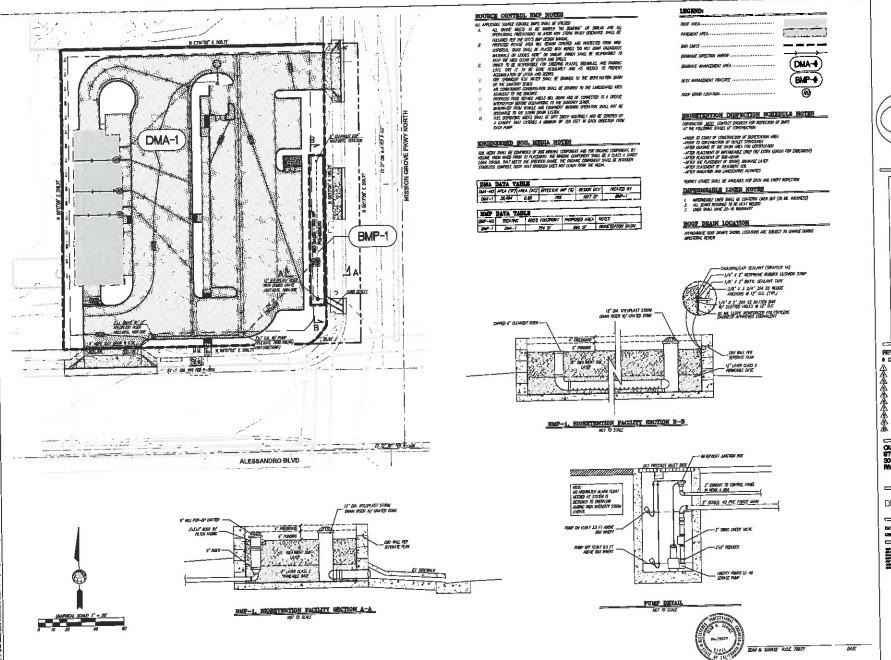
PRELIMINARY GRADING PLAN

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4 NG YIEWRIDGE AVE. SUITE B
53 N DIEGO. CA 92123
PH: (858) 634-8627) VIEW ! DIEG: :(858) !



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DMA MAP

CHIM PROJECT 6



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NOTICE OF PUBLIC HEARING RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact ALUC Planner Paul Rull at (951) 955-6893. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The City of Riverside should be contacted on non-ALUC issues. For more information please contact City of Riverside Planner Ms. Candice Assadzadeh at (951) 826-5667.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to prull@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please telephone Barbara Santos at (951) 955-5132.

PLACE OF HEARING: Riverside County Administration Center

4080 Lemon Street, 1st Floor Board Chambers

Riverside California

DATE OF HEARING: May 13, 2021

TIME OF HEARING: 9:30 A.M.

Pursuant to Executive Order N-25-20, this meeting will be conducted by teleconference and at the Place of Hearing, as listed above. Public access to the meeting location will be allowed, but limited to comply with the Executive Order. Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org

CASE DESCRIPTION:

ZAP1457MA21 – Eugene Marini (Representative: Omega Engineering Consultants) – City of Riverside Case No. PR-2021-001023 (General Plan Amendment, Rezone, Conditional Use Permit, Design Review), a proposal to construct a 3,590 square foot drive-thru carwash building on 0.89 acres, located on the northwest corner of Mission Grove Parkway and Alessandro Boulevard. The applicant also proposes amending the site's General Plan land use designation from Office (O) to Commercial (C), and changing the site's zoning from Office (O) to Commercial General (CG). (Airport Compatibility Zone C2 of the March Air Reserve Base/Inland Port Airport Influence Area).



RIVERSIDE COUNTY

AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW 3/4/2021 ZAP1457MA71 _____ DATE SUBMITTED: __ APPLICANT / REPRESENTATIVE / PROPERTY OWNER CONTACT INFORMATION (619) 488-6293 Phone Number Omega Engineering Consultants Applicant Email max@omega-consultants.com 4340 Viewridge Ave Suite B, San Diego, CA 92123 Mailing Address (858) 281-6091 Phone Number Eugene Marini Representative Eugene@KAenterprises.net 5820 Oberlin Drive, Suite 201, San Diego, CA 92121 Email Mailing Address (858) 281-6091 Phone Number Eugene Marini Property Owner Eugene@KAenterprises.net 5820 Oberlin Drive, Suite 201, San Diego, CA 92121 Mailing Address LOCAL JURISDICTION AGENCY (951) 826-5667 **Phone Number** City of Riverside Local Agency Name Email CAssadzadeh@riversideca.gov Candice Assadzadeh **Staff Contact** Case Type **Mailing Address** General Plan / Specific Plan Amendment Zoning Ordinance Amendment Subdivision Parcel Map / Tentative Tract Use Permit Planning Case PR-2021-001023 (GPA, RZ, CUP, DR, VR) Local Agency Project No Site Plan Review/Plot Plan ☐ Other **PROJECT LOCATION** Attach an accurately scaled map showing the relationship of the project site to the airport boundary and runways 306 East Alessandro Boulevard, Riverside, CA 92508 Street Address 43,459(Gross) 38,484(Net) **Gross Parcel Size** 272-060-004-7 Assessor's Parcel No. Nearest Airport and Map of the Alessandro Tract Subdivision Name distance from Air-March Air Reserve Base - 2.94 Miles Lot 4, Block 4, Book 6, Page 13 of Maps port Lot Number **PROJECT DESCRIPTION** If applicable, attach a detailed site plan showing ground elevations, the location of structures, open spaces and water bodies, and the heights of structures and trees; include additional project description data as needed Office Existing Land Use (describe)

March

Proposed Land Use (describe)	Commercial Retail									
(2.2.2.2)										
For Residential Uses	Number of Parcels or Units on Site Hours of Operation 7AM-7P		ts)							
For Other Land Uses (See Appendix C)	Number of People on Site 57 Method of Calculation	Maximum Number 3 employees, 5 cars in	176 carwash (at max capacity), 15 c	ars in queue, 16 cars (1 in ea	ach vacuum stall).					
	interior or equation.	At 1.5 people per car we get 57 people on site.								
Height Data	Site Elevation (above mean sea le	vel)	1603		ft.					
Height Dota	Height of buildings or structures		29.0	29.0						
Flight Hazards	Does the project involve any char confusing lights, glare, smoke, or	acteristics which could c other electrical or visual	reate electrical interference, hazards to aircraft flight?	Yes No						
	If yes, describe									
1										

- A. NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME: Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:
 - 1..... Completed ALUC Application Form
 - 1..... ALUC fee payment
 - 1..... Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
 - 1. Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
 - 1..... CD with digital files of the plans (pdf)
 - 1..... Vicinity Map (8.5x11)
 - 1..... Detailed project description
 - 1. Local jurisdiction project transmittal
 - 3. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
 - 3. . . . Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. (Only required if the project is scheduled for a public hearing Commission meeting)

COUNTY OF RIVERSIDE AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM:

3.4

HEARING DATE:

May 13, 2021

CASE NUMBER:

ZAP1458MA21 - MVRP PV (Representative: Sunworks

Inc.)

APPROVING JURISDICTION:

City of Moreno Valley

JURISDICTION CASE NO:

PEN20-0198 (Plot Plan)

LAND USE PLAN:

2014 March Air Reserve Base/Inland Port Airport Land Use

Compatibility Plan

Airport Influence Area:

March Air Reserve Base

Land Use Policy:

Zone C1

Noise Levels:

60 - 65 CNEL contour

MAJOR ISSUES:

None

RECOMMENDATION: Staff recommends that the Commission <u>CONTINUE</u> the matter to the June 10, 2021 meeting, pending completion of the Air Force review of the project.

PROJECT DESCRIPTION: A proposal to construct a 5,588 square foot rooftop solar panel system on an existing industrial building on 1.62 acres.

PROJECT LOCATION: The site is at 14114 Business Center Drive, northerly of Golden Crest Drive, westerly of Elsworth Street, and southerly of Alessandro Boulevard, in the City of Moreno Valley, approximately 5,675 feet northerly of the northerly end of Runway 14-32 at March Air Reserve Base.

BACKGROUND:

Non-Residential Land Use Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zone C1 which limits average intensity to 100 people per acre and 250 people per single acre. The proposed rooftop solar panels will not generate any occupancy.

March Air Reserve Base/United States Air Force Input: Given that the project site is located in Zone C1 northerly of the northerly runway at March Air Reserve Base, the March Air Reserve Base staff

was notified of the proposal to add rooftop solar panels, and sent a solar glare hazard analysis study for their review. As of the time this staff report was prepared, no comments have been received from the Air Force regarding this project.

<u>Flight Hazard Issues</u>: Structure height, electrical interference, and reflectivity/glare are among the issues that solar panels in the airport influence area must address. The project's 5,588 square foot photovoltaic (PV) panel structures would be located on the rooftop of the existing industrial building within Compatibility Zone C1.

Glint and Glare/Reflectivity

Based on the Federal Aviation Administration's Interim Policy for Review of Solar Energy System Projects on Federally Obligated Airports, no glare potential or low potential for temporary afterimage ("green" level) are acceptable levels of glare on final approach (within 2 miles from end of runway) for solar facilities located on airport property. However, potential for temporary afterimage" ("yellow" level) and potential for permanent eye damage ("red" level) are not acceptable levels of glare on final approach. No glare is permitted at air traffic control towers.

The project proposes 5,588 square feet of solar panels on the existing building rooftop with a fixed tilt of 10 degrees with no rotation, and an orientation of 182 degrees. The applicant has submitted a glare analysis utilizing the web-based Forge Solar, a copy of which is attached hereto. The analysis was based on a 2 mile straight in approach (as per FAA Interim Policy standards) to runways 14 and 32, and also based on the traffic patterns as identified by March Air Reserve Base staff (Runway 12/30 General Aviation, Runway 14/32 General Aviation, Runway 14/32 C-17/KC-135, Runway 14/32 Overhead). The analysis utilized a glide slope approach of 5.0 degrees. No glare would affect the Air Traffic Control Tower.

The analysis concluded that no glare would occur on the 2 mile approach to runways 14 and 32. However, some potential for glare was identified within the Air Force traffic pattern. Evaluation of the Air Force traffic patterns indicates that the panels would result in low potential for temporary after-image ("green" level glare). All times are in standard time.

Runway 14/32 C-17/KC-135 traffic pattern:

- Runway 32 Crosswind portion, totaling 168 minutes of "green" level glare, lasting up to 5 minutes a day, in February to March, and October to November from 6:00 a.m. to 7:00 a.m.
- Runway 32 Upwind portion, totaling 799 minutes of "green" level glare, lasting up to 5 minutes a day, between October to April from 6:30 a.m. to 7:30 a.m.

Runway 12/30 General Aviation traffic pattern:

• Runway 30 Downwind portion, totaling 1,331 minutes of "green" level glare, lasting up to 20 minutes a day, between May to August from 5:00 p.m. to 6:00 p.m.

Staff Report Page 3 of 5

The total of 2,298 minutes of "green" level glare represents less than 1 percent of total day light time.

Electrical and Communication Interference

The applicant has indicated that they do not plan to utilize equipment that would interfere with aircraft communications. The PV panels themselves present little risk of interfering with radar transmission due to their low profiles. In addition, solar panels do not emit electromagnetic waves over distances that could interfere with radar signal transmissions, and any electrical facilities that do carry concentrated current will be buried beneath the ground and away from any signal transmission. There are no radar transmission or receiving facilities within the site.

<u>Prohibited and Discouraged Uses:</u> Glare from solar panels could potentially constitute a hazard to flight. However, based on the solar glare hazard analysis provided, the glare experienced would result in a low potential for temporary after-image ("green" level) which has been determined by the Federal Aviation Administration (FAA) to be an acceptable level for solar facilities on airports. Therefore, the hazard potential is low. Staff has included conditions to remedy unanticipated situations.

<u>Noise:</u> The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site within the 60-65 CNEL range from aircraft noise. The proposed solar panels are a non-noise sensitive use, therefore no mitigation measures are necessary.

Part 77: The elevation of Runway 14-32 at its northerly terminus is 1,535 feet above mean sea level (1,535 feet AMSL). At a distance of approximately 5,675 feet from the runway to the site, Federal Aviation Administration (FAA) review would be required for any structures with top of roof elevation exceeding 1,593 feet AMSL. The site's finished floor elevation is 1,561 feet AMSL and the existing building height is 30 feet. The establishment of the proposed rooftop solar panels would increase the maximum elevation by 1 foot, for a top point elevation of 1,592 feet AMSL. Therefore, review by the FAA Obstruction Evaluation Service is not required.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

CONDITIONS:

- 1. Any outdoor lighting installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
- 2. The following uses/activities are not included in the proposed project and shall be prohibited at this site.
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight or circling climb following takeoff or toward an aircraft engaged in a

- straight or circling final approach toward a landing at an airport, other than a DoD or FAA-approved navigational signal light or visual approach slope indicator.
- (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport to the extent as to result in a potential for temporary after-image greater than the low ("green") level.
- (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
- (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
- (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, congregate care facilities, places of assembly (including but not limited to places of worship and theaters)
- (f) Highly noise-sensitive outdoor nonresidential uses. Examples of noise-sensitive outdoor nonresidential uses that are prohibited include, but are not limited to, major spectator-oriented sports stadiums, amphitheaters, concert halls and drive-in theaters.
- (g) Other Hazards to Flight
- 3. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property and be recorded as a deed notice.
- 4. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
- 5. The maximum height of the building, including all roof-mounted equipment including solar panels, if any, shall be limited to 31 feet, and the maximum top point elevation shall not exceed 1,593 feet above mean sea level unless a "Determination of No Hazard to Air Navigation" letter authorizing a higher top point elevation has been issued by the Federal Aviation Administration Obstruction Evaluation Service.

- 6. All solar arrays installed on the project site shall consist of smooth glass photovoltaic solar panels without anti-reflective coating, a fixed tilt of 10 degrees and orientation of 182 degrees. Solar panels shall be limited to a total of 5,588 square feet, and the locations and coordinates shall be as specified in the glare study. Any deviation from these specifications (other than reduction in square footage of panels), including change in orientation, shall require a new solar glare analysis to ensure that the amended project does not result in any glare impacting the air traffic control tower or creation of any "yellow" or "red" level glare in the flight paths, and shall require a new hearing by the Airport Land Use Commission.
- 7. In the event that any glint, glare, or flash affecting the safety of air navigation occurs as a result of project operation, upon notification to the airport operator of an event, the airport operator shall notify the project operator in writing. Within 30 days of written notice, the project operator shall be required to promptly take all measures necessary to eliminate such glint, glare, or flash. An "event" includes any situation that results in an accident, incident, "near-miss," or specific safety complaint regarding an in-flight experience to the airport operator or to federal, state, or county authorities responsible for the safety of air navigation. The project operator shall work with the airport operator to prevent recurrence of the incidence. Suggested measures may include, but are not limited to, changing the orientation and/or tilt of the source, covering the source at the time of day when events of glare occur, or wholly removing the source to diminish or eliminate the source of the glint, glare, or flash. For each such event made known to the project operator, the necessary remediation shall only be considered to have been fulfilled when the airport operator states in writing that the situation has been remediated to the airport operator's satisfaction.
- 8. In the event that any electrical interference affecting the safety of air navigation occurs as a result of project operation, upon notification to the airport operator of an event, the airport operator shall notify the project operator in writing. Within 30 days of written notice, the project operator shall be required to promptly take all measures necessary to eliminate such interference. An "event" includes any situation that results in an accident, incident, "nearmiss," report by airport personnel, or specific safety complaint to the airport operator or to federal, state, or county authorities responsible for the safety of air navigation. The project operator shall work with the airport operator to prevent recurrence of the event. For each such event made known to the project operator, the necessary remediation shall only be considered to have been fulfilled when the airport operator states in writing that the situation has been remediated to the airport operator's satisfaction.

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NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b)



FORGESOLAR GLARE ANALYSIS

Project: MRVP-30

Proposed 104kW PV rooftop system near March AFB in Moreno Valley, CA.

Site configuration: Flight Path Analysis

Analysis conducted by Lawrence Lawlor (it@sunworksusa.com) at 19:22 on 29 Mar, 2021.

U.S. FAA 2013 Policy Adherence

The following table summarizes the policy adherence of the glare analysis based on the 2013 U.S. Federal Aviation Administration InterIm Policy 78 FR 63276. This policy requires the following criteria be met for solar energy systems on airport property:

- No "yellow" glare (potential for after-image) for any flight path from threshold to 2 miles
- No glare of any kind for Air Traffic Control Tower(s) ("ATCT") at cab height.
- * Default analysis and observer characteristics (see list below)

ForgeSolar does not represent or speak officially for the FAA and cannot approve or deny projects. Results are informational only.

COMPONENT	STATUS	DESCRIPTION
Analysis parameters	PASS	Analysis time interval and eye characteristics used are acceptable
2-mile flight path(s)	PASS	Flight path receptor(s) do not receive yellow glare
ATCT(s)	PASS	Receptor(s) marked as ATCT do not receive glare

Default glare analysis parameters and observer eye characteristics (for reference only):

Analysis time interval: 1 minute
Ocular transmission coefficient: 0.5

Pupil diameter: 0.002 meters
Eye focal length: 0.017 meters

* Sun subtended angle: 9.3 milliradians

FAA Policy 78 FR 63276 can be read at https://www.federalregister.gov/d/2013-24729

SITE CONFIGURATION

Analysis Parameters

DNI: peaks at 1,000.0 W/m^2

Time interval: 1 min Ocular transmission coefficient: 0.5

Pupil diameter: 0.002 m Eye focal length: 0.017 m Sun subtended angle: 9.3

mrad

Site Config ID: 51748.8901



PV Array(s)

Name: PV Array 1

Axis tracking: Fixed (no rotation)

Tilt: 10.0°

Orientation: 182.0° Rated power: 104.0 kW

Panel material: Smooth glass without AR coating

Reflectivity: Vary with sun

Slope error: correlate with material



Vertex	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
1	33.915051	-117.272082	1561.10	29.00	1590.10
2	33.915049	-117.271659	1561.34	28.76	1590.10
3	33.914635	-117.271672	1562.85	27.25	1590.10
4	33.914636	-117.272088	1560.08	30.02	1590.10

Flight Path Receptor(s)

Name: C/KC Rwy 14 Base Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.922394	-117.325047	1500.07	1500.07	3000.15
Two-mile	33.931244	-117.309014	1500.07	1500.07	3000.15

Name: C/KC Rwy 14 Crosswind

Description: None Threshold height: 0 ft Direction: 314.8° Gilde slope: 5.0°

Pliot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.821961	-117.228367	1500.07	1500.07	3000.15
Two-mile	33.813147	-117.244350	1500.07	1500.07	3000.15

Name: C/KC Rwy 14 Downwind

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.819225	-117.262269	1500.07	1500.07	3000.15
Two-mile	33.908131	-117.325528	1500.07	1500.07	3000.15

Name: C/KC Rwy 14 Final Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.925156	-117.291061	1500.07	1500.07	3000.15
Two-mile	33.896431	-117.270636	1500.07	0.00	1500.07

Name: C/KC Rwy 14 Upwind

Description: None Threshold height: 0 ft Direction: 314.8° Gilde slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.864994	-117.248281	1500.07	0.00	1500.07
Two-mile	33.836269	-117.227869	1500.07	1500.07	3000.15

Name: C/KC Rwy 32 Base Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.813147	-117.244350	1500.07	1500.07	3000.15
Two-mile	33.821961	-117.228367	1500.07	1500.07	3000.15

Name: C/KC Rwy 32 Crosswind

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.931244	-117.309014	1500.07	1500.07	3000.15
Two-mile	33.922394	-117.325047	1500.07	1500.07	3000.15

Name: C/KC Rwy 32 Downwind

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.908131	-117.325528	1500.07	1500.07	3000.15
Two-mile	33.819225	-117.262269	1500.07	1500.07	3000.15

Name: C/KC Rwy 32 Final Description: None Threshold helght: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°

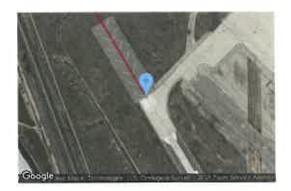


Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.836269	-117.227869	1500.07	1500.07	3000.15
Two-mile	33.864994	-117.248281	1500.07	0.00	1500.07

Name: C/KC Rwy 32 Upwind

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.896431	-117.270636	1500.07	0.00	1500.07
Two-mile	33.925156	-117.291061	1500.07	1500.07	3000.15

Name: GA Rwy 12 Base Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.910322	-117.264967	1500.07	1300.06	2800.14
Two-mile	33.905592	-117.270622	1500.07	1300.06	2800.14

Name: GA Rwy 12 Crosswind

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.876081	-117.235119	1500.07	1300.06	2800.14
Two-mile	33.880814	-117.229467	1500.07	1300.06	2800.14

Name: GA Rwy 12 Downwind

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.887897	-117.229483	1500.07	1300.06	2800.14
Two-mile	33.910333	-117.256469	1500.07	1300.06	2800.14

Name: GA Rwy 12 Final Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.898508	-117.270608	1500.07	1300.06	2800.14
Two-mile	33.890258	-117.260681	1500.07	0.00	1500.07

Name: GA Rwy 14 Base Description: None Threshold height: 0 ft Direction: 314.8° Glide stope: 5.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.904833	-117.292903	1500.07	1500.07	3000.15
Two-mile	33.908242	-117.286017	1500.07	1500.07	3000.15

Name: GA Rwy 14 Crosswind

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.848078	-117.243236	1500.07	1500.07	3000.15
Two-mile	33.844669	-117.250119	1500.07	1500.07	3000.15

Name: GA Rwy 14 Downwind

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.846422	-117.258344	1500.07	1500.07	3000.15
Two-mile	33.897972	-117.295011	1500.07	1500.07	3000.15

Name: GA Rwy 14 Final Description: None Threshold helght: 0 ft Direction: 314.8° Glide slope: 5.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.906486	-117.277783	1500.07	1500.07	3000.15
Two-mile	33.896431	-117.270636	1500.07	0.00	1500.07

Name: GA Rwy 14 Upwind Description: None Threshold helght: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.864994	-117.248281	1500.07	0.00	1500.07
Two-mile	33.854942	-117.241136	1500.07	1500.07	3000.15

Name: GA Rwy 30 Base Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azlmuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.880814	-117.229467	1500.07	1300.06	2800.14
Two-mile	33.876081	-117.235119	1500.07	1300.06	2800.14

Name: GA Rwy 30 Crosswind

Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.905592	-117.270622	1500.07	1300.06	2800.14
Two-mile	33.910322	-117.264967	1500.07	1300.06	2800.14

Name: GA Rwy 30 Downwind

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.910333	-117.256469	1500.07	1300.06	2800.14
Two-mile	33.887897	-117.229483	1500.07	1300.06	2800.14

Name: GA Rwy 30 Final Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.876069	-117.243611	1500.07	1300.06	2800.14
Two-mile	33.884319	-117.253536	1500.07	0.00	1500.07

Name: GA Rwy 30 Upwind Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.890258	-117.260681	1500.07	0.00	1500.07
Two-mile	33 898508	-117 270608	1500.07	1300.06	2800.14

Name: GA Rwy 32 Base Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°

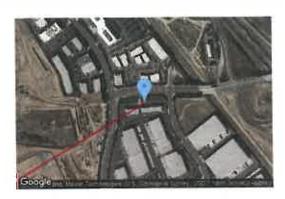


Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Helght above ground (ft)	Total elevation (ft)
Threshold	33.844669	-117.250119	1500.07	1500.07	3000.15
Two-mile	33.848078	-117.243236	1500.07	1500.07	3000.15

Name: GA Rwy 32 Crosswind

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

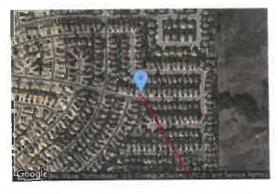
Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.908242	-117.286017	1500.07	1500.07	3000.15
Two-mile	33.904833	-117.292903	1500.07	1500.07	3000.15

Name: GA Rwy 32 Downwind

Description: None
Threshold height: 0 ft
Direction: 314.8°
Glide slope: 5.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.897972	-117.295011	1500.07	1500.07	3000.15
Two-mile	33 846422	-117.258344	1500.07	1500.07	3000.15

Name: GA Rwy 32 Final Description: None Threshold height: 0 ft Direction: 314.8° Gilde slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.854942	-117.241136	1500.07	1500.07	3000.15
Two-mile	33.864994	-117.248281	1500.07	0.00	1500.07

Name: GA Rwy R32 Upwind

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pliot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.896431	-117.270636	1500.07	0.00	1500.07
Two-mile	33.906486	-117.277783	1500.07	1500.07	3000.15

Name: OHead Rwy 14 Downwind

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.863564	-117.293808	1500.07	2000.10	3500.17
Two-mile	33.908131	-117.325528	1500.07	2000.10	3500.17

Name: OHead Rwy 14 Final

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.925156	-117.291061	1500.07	2000.10	3500.17
Two-mile	33.896431	-117.270636	1500.07	0.00	1500.07

Name: OHead Rwy 14 Initial

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.968036	-117.322128	1500.07	2000.10	3500.17
Two-mile	33.880706	-117.259453	1500.07	2000.10	3500.17

Name: OHead Rwy 32 Downwind

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.863564	-117.293808	1500.07	2000.10	3500.17
Two-mile	33.819225	-117.262269	1500.07	2000.10	3500.17

Name: OHead Rwy 32 Final

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.836269	-117.227869	1500.07	2000.10	3500.17
Two-mile	33.864994	-117.248281	1500.07	0.00	1500.07

Name: OHead Rwy 32 Initial

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.793375	-117.196878	1500.07	2000.10	3500.17
Two-mile	33.880706	-117.259453	1500.07	2000.10	3500.17

Name: Rwy 12 Upwind (GA)

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

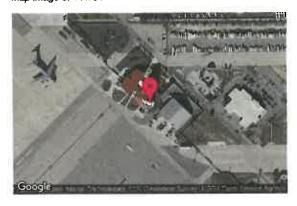


Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.884319	-117.253536	1500.07	0.00	1500.07
Two-mile	33.876069	-117.243611	1500.07	1300.06	2800.14

Discrete Observation Receptors

Name	ID	Latitude (°)	Longitude (°)	Elevation (ft)	Height (ft)
1-ATCT	1	33.891564	-117.251171	1508.07	118.01

Map image of 1-ATCT



GLARE ANALYSIS RESULTS

Summary of Glare

PV Array Name	Tilt	Orient	"Green" Glare	"Yellow" Glare	Energy
	(°)	(°)	min	min	kWh
PV Array 1	10.0	182.0	2,298	0	232,700.0

Total annual glare received by each receptor

Receptor	Annual Green Glare (min)	Annual Yellow Glare (min)
C/KC Rwy 14 Base	o	0
C/KC Rwy 14 Crosswind	o	o
C/KC Rwy 14 Downwind	o	o
C/KC Rwy 14 Final	O	O
C/KC Rwy 14 Upwind	o	О
C/KC Rwy 32 Base	О	0
C/KC Rwy 32 Crosswind	168	0
C/KC Rwy 32 Downwind	0	o
C/KC Rwy 32 Final	0	0
C/KC Rwy 32 Upwind	799	0
GA Rwy 12 Base	0	0

Receptor	Annual Green Glare (min)	Annual Yellow Glare (min)
GA Rwy 12 Crosswind	0	0
GA Rwy 12 Downwind	o	0
GA Rwy 12 Final	O	0
GA Rwy 14 Base	0	0
GA Rwy 14 Crosswind	0	0
GA Rwy 14 Downwind	0	o
GA Rwy 14 Final	0	0
GA Rwy 14 Upwind	0	0
GA Rwy 30 Base	o	0
GA Rwy 30 Crosswind	0	0
GA Rwy 30 Downwind	1331	o
GA Rwy 30 Final	0	0
GA Rwy 30 Upwind	0	0
GA Rwy 32 Base	0	0
GA Rwy 32 Crosswind	0	o
GA Rwy 32 Downwind	0	0
GA Rwy 32 Final	0	0
GA Rwy R32 Upwind	0	0
OHead Rwy 14 Downwind	0	0
OHead Rwy 14 Final	0	0
OHead Rwy 14 Initial	0	0
OHead Rwy 32 Downwind	0	0
OHead Rwy 32 Final	0	0
OHead Rwy 32 Initial	0	0
Rwy 12 Upwind (GA)	o	0
1-ATCT	0	0

Results for: PV Array 1

Receptor	Green Glare (min)	Yellow Glare (min)
C/KC Rwy 14 Base	0	0
C/KC Rwy 14 Crosswind	0	0
C/KC Rwy 14 Downwind	0	0
C/KC Rwy 14 Final	0	0
C/KC Rwy 14 Upwind	o	0
C/KC Rwy 32 Base	0	0
C/KC Rwy 32 Crosswind	168	0
C/KC Rwy 32 Downwind	0	0
C/KC Rwy 32 Final	0	0
C/KC Rwy 32 Upwind	799	0

GA Rwy 12 Base 0 0 GA Rwy 12 Crosswind 0 0 GA Rwy 12 Downwind 0 0	
CA Dure 10 Deservine	
GA Rwy 12 Downwind 0 0	
GA Rwy 12 Final 0 0	
GA Rwy 14 Base 0 0	
GA Rwy 14 Crosswind 0 0	
GA Rwy 14 Downwind 0 0	
GA Rwy 14 Final 0 0	
GA Rwy 14 Upwind 0 0	
GA Rwy 30 Base 0 0	
GA Rwy 30 Crosswind 0 0	
GA Rwy 30 Downwind 1331 0	
GA Rwy 30 Final 0	
GA Rwy 30 Upwind 0 0	
GA Rwy 32 Base 0 0	
GA Rwy 32 Crosswind 0 0	
GA Rwy 32 Downwind 0 0	
GA Rwy 32 Final 0	
GA Rwy R32 Upwind 0 0	
OHead Rwy 14 Downwind 0 0	ı
OHead Rwy 14 Final 0 0	ı
OHead Rwy 14 Initial 0 0	i
OHead Rwy 32 Downwind 0 0	I
OHead Rwy 32 Final 0	1
OHead Rwy 32 Initial 0 0	
Rwy 12 Upwind (GA) 0 0	1
1-ATCT 0 0	1

Flight Path: C/KC Rwy 14 Base

0 minutes of yellow glare 0 minutes of green glare

Flight Path: C/KC Rwy 14 Crosswind

0 minutes of yellow glare 0 minutes of green glare

Flight Path: C/KC Rwy 14 Downwind

0 minutes of yellow glare 0 minutes of green glare

Flight Path: C/KC Rwy 14 Final

0 minutes of yellow glare0 minutes of green glare

Flight Path: C/KC Rwy 14 Upwind

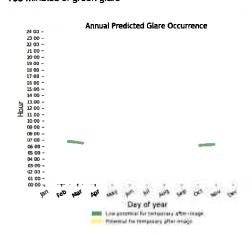
0 minutes of yellow glare 0 minutes of green glare

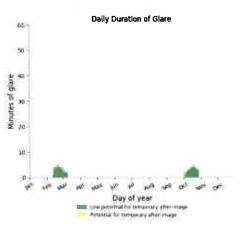
Flight Path: C/KC Rwy 32 Base

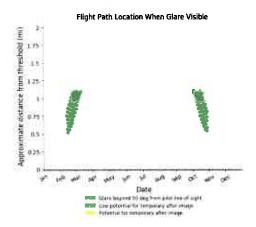
0 minutes of yellow glare 0 minutes of green glare

Flight Path: C/KC Rwy 32 Crosswind

0 minutes of yellow glare 168 minutes of green glare







Flight Path: C/KC Rwy 32 Downwind

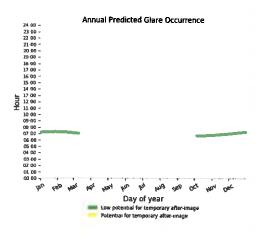
0 minutes of yellow glare 0 minutes of green glare

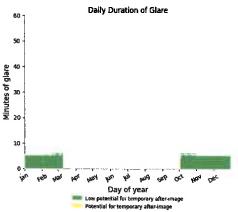
Flight Path: C/KC Rwy 32 Final

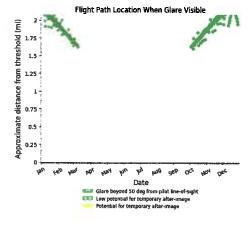
0 minutes of yellow glare 0 minutes of green glare

Flight Path: C/KC Rwy 32 Upwind

0 minutes of yellow glare 799 minutes of green glare







Flight Path: GA Rwy 12 Base

0 minutes of yellow glare 0 minutes of green glare

Flight Path: GA Rwy 12 Crosswind

0 minutes of yellow glare 0 minutes of green glare

Flight Path: GA Rwy 12 Downwind

0 minutes of yellow glare 0 minutes of green glare

Flight Path: GA Rwy 12 Final

0 minutes of yellow glare 0 minutes of green glare

Flight Path: GA Rwy 14 Base

0 minutes of yellow glare 0 minutes of green glare

Flight Path: GA Rwy 14 Crosswind

0 minutes of yellow glare 0 minutes of green glare

Flight Path: GA Rwy 14 Downwind

0 minutes of yellow glare 0 minutes of green glare

Flight Path: GA Rwy 14 Final

0 minutes of yellow glare 0 minutes of green glare

Flight Path: GA Rwy 14 Upwind

0 minutes of yellow glare 0 minutes of green glare

Flight Path: GA Rwy 30 Base

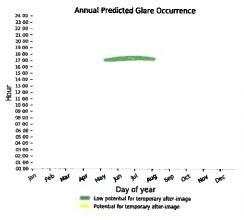
0 minutes of yellow glare 0 minutes of green glare

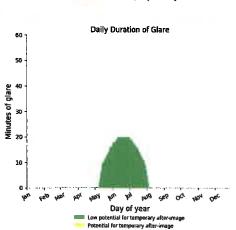
Flight Path: GA Rwy 30 Crosswind

0 minutes of yellow glare

Flight Path: GA Rwy 30 Downwind

0 minutes of yellow glare 1331 minutes of green glare







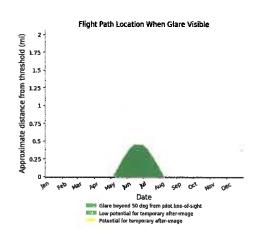
0 minutes of yellow glare 0 minutes of green glare

Flight Path: GA Rwy 30 Upwind

0 minutes of yellow glare 0 minutes of green glare

Flight Path: GA Rwy 32 Base

0 minutes of yellow glare 0 minutes of green glare



Flight Path: GA Rwy 32 Crosswind

0 minutes of yellow glare 0 minutes of green glare

Flight Path: GA Rwy 32 Downwind

0 minutes of yellow glare 0 minutes of green glare

Flight Path: GA Rwy 32 Final

0 minutes of yellow glare 0 minutes of green glare

Flight Path: GA Rwy R32 Upwind

0 minutes of yellow glare 0 minutes of green glare

Flight Path: OHead Rwy 14 Downwind

0 minutes of yellow glare 0 minutes of green glare

Flight Path: OHead Rwy 14 Final

0 minutes of yellow glare 0 minutes of green glare

Flight Path: OHead Rwy 14 Initial

0 minutes of yellow glare 0 minutes of green glare

Flight Path: OHead Rwy 32 Downwind

0 minutes of yellow glare 0 minutes of green glare

Flight Path: OHead Rwy 32 Final

0 minutes of yeliow glare 0 minutes of green glare

Flight Path: OHead Rwy 32 Initial

0 minutes of yellow glare

0 minutes of green glare

Flight Path: Rwy 12 Upwind (GA)

0 minutes of yellow glare 0 minutes of green glare

Point Receptor: 1-ATCT

0 minutes of yellow glare0 minutes of green glare

Assumptions

"Green" glare is glare with low potential to cause an after-image (flash blindness) when observed prior to a typical blink response time.

"Yellow" glare is glare with potential to cause an after-image (flash blindness) when observed prior to a typical blink response time.

Times associated with glare are denoted in Standard time. For Daylight Savings, add one hour.

Glare analyses do not account for physical obstructions between reflectors and receptors. This includes buildings, tree cover and geographic obstructions.

Several calculations utilize the PV array centroid, rather than the actual glare spot location, due to algorithm limitations. This may affect results for large PV footprints. Additional analyses of array sub-sections can provide additional information on expected glare.

The subtended source angle (glare spot size) is constrained by the PV array footprint size. Partitioning large arrays into smaller sections will reduce the maximum potential subtended angle, potentially impacting results if actual glare spots are larger than the sub-array size. Additional analyses of the combined area of adjacent sub-arrays can provide more information on potential glare hazards. (See previous point on related limitations.)

Glare locations displayed on receptor plots are approximate. Actual glare-spot locations may differ.

Glare vector plots are simplified representations of analysis data. Actual glare emanations and results may differ.

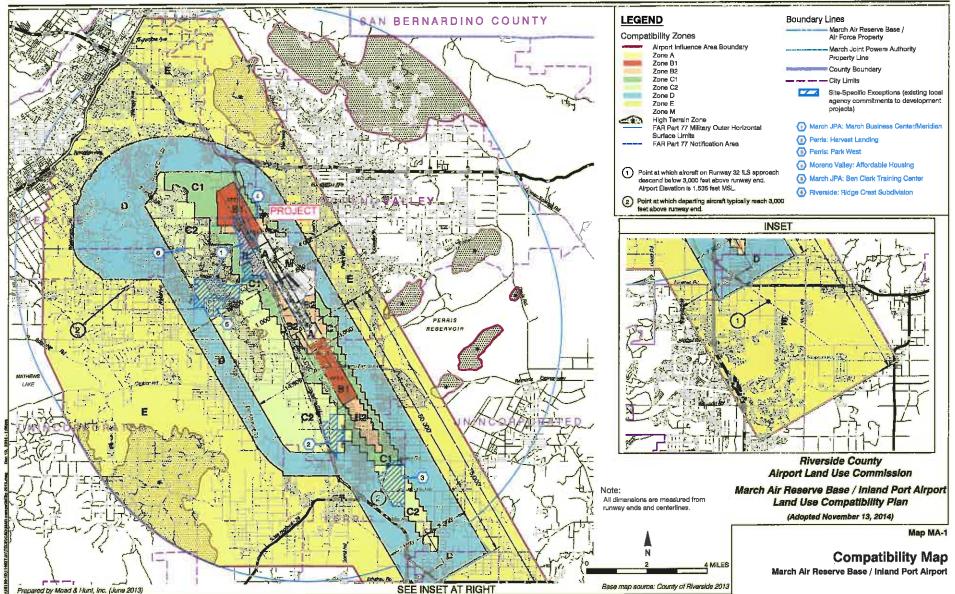
The glare hazard determination relies on several approximations including observer eye characteristics, angle of view, and typical blink response time. Actual results and glare occurrence may differ.

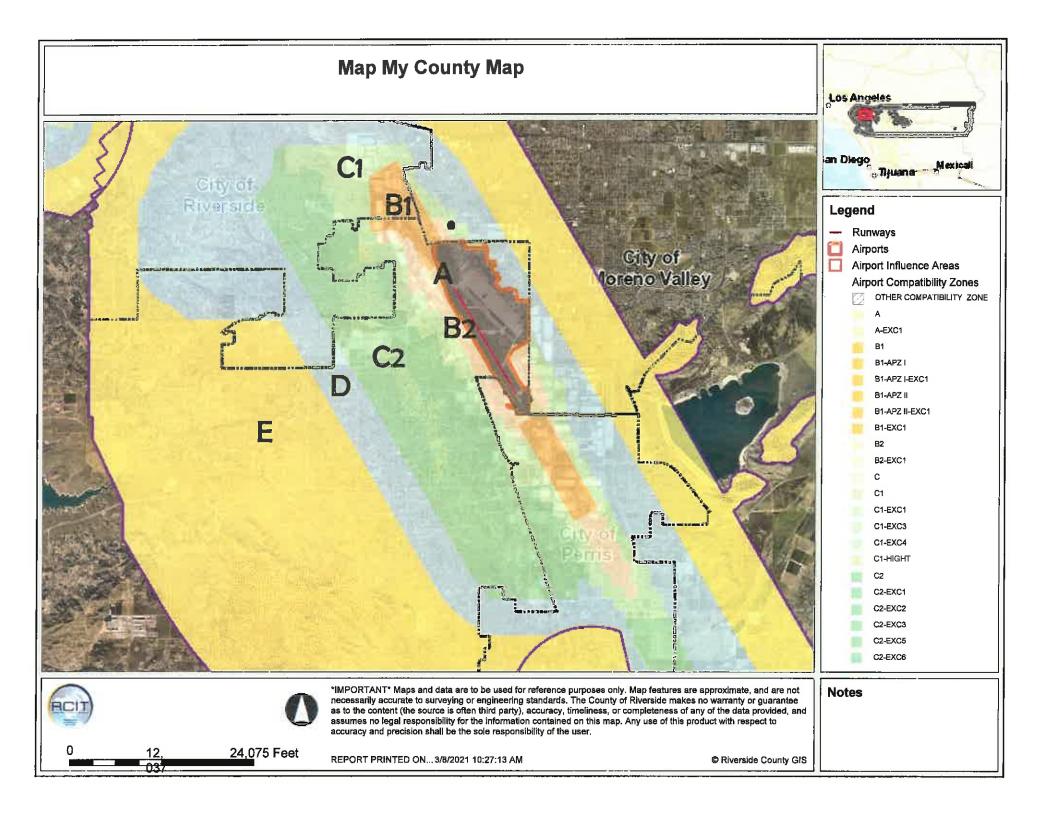
Hazard zone boundaries shown in the Giare Hazard plot are an approximation and visual aid based on aggregated research data. Actual ocular impact outcomes encompass a continuous, not discrete, spectrum.

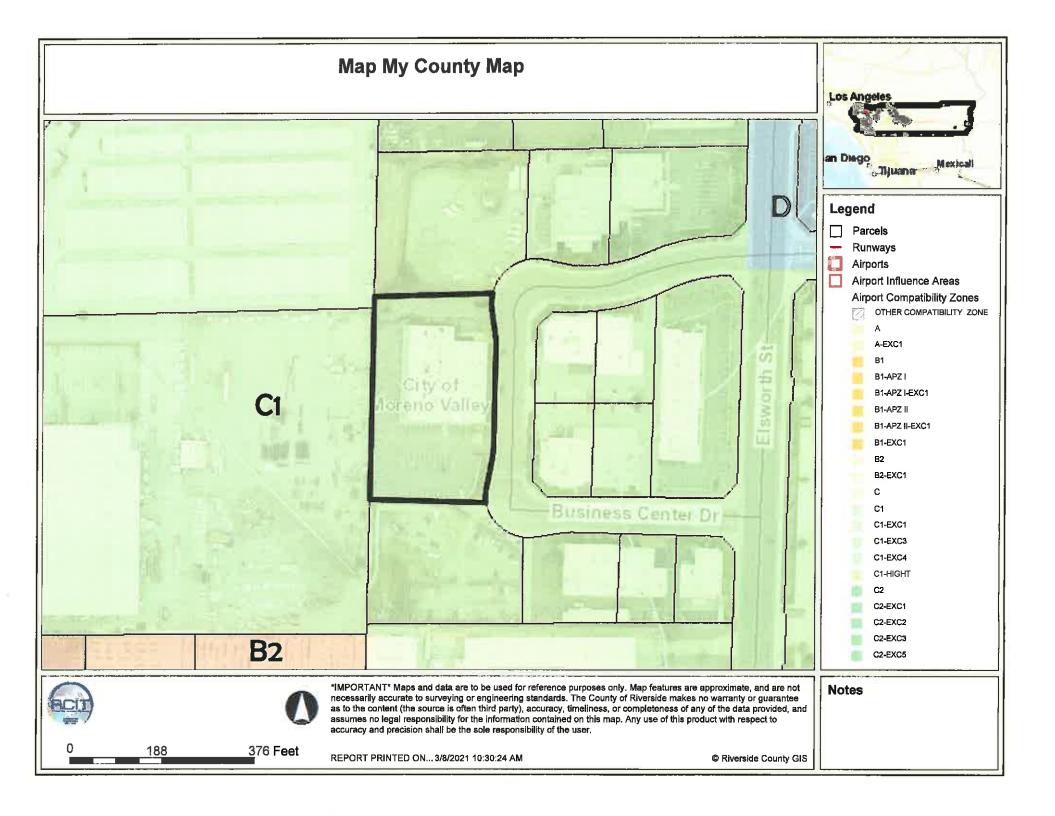
Refer to the Help page at www.forgesolar.com/help/ for assumptions and limitations not listed here.

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Legend

- Parcels
- County Centerlines
 Blueline Streams
 City Areas
 World Street Map



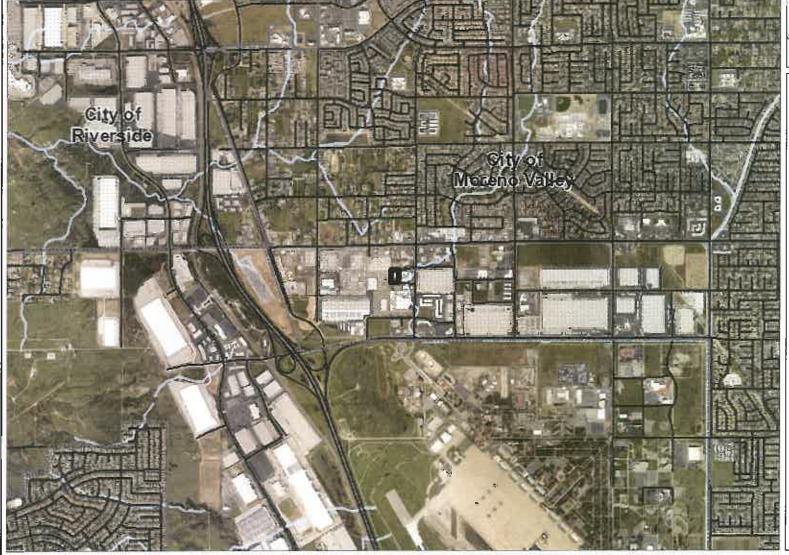


IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

188 376 Feet

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Legend

County Centerlines
 Blueline Streams
 City Areas
 World Street Map





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3, 6,019 Feet

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Legend

- County Centerlines
 Blueline Streams
- City Areas
 World Street Map

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1, 3,009 Feet

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Legend

- Parcels County Centerline Names
- **County Centerlines** Blueline Streams City Areas World Street Map



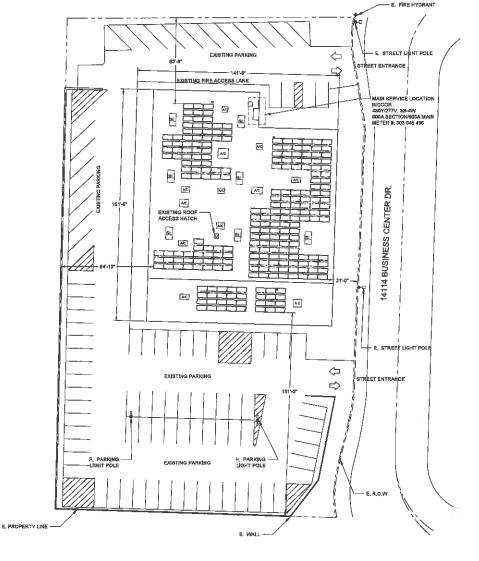


IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to

376 Feet

accuracy and precision shall be the sole responsibility of the user.

REPORT PRINTED ON... 3/8/2021 10:32:29 AM



SITE NOTES

INTERIOR PATHWAYS, INTERIOR PARMIAYS SHALL BE PROVIDED BETWEEN ARRAY SECTIONS TO MEET THE FOLLOWING RECUREMENTS

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A PATHRAY NOT LESS THAN 4FT NOTE IN A STRAIGHT LINE TO ROOF STANDPIPES OF VENTILATION HATCHES.

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LEGAL DESCRIPTION

297-210-026 STREE ADDRESS: 14114 RUSINESS CENTER DR. COMMERCIAL - DIFFICE BODK: PAGE: TRACT & LUT: YEAR BLOLT: 188

PARCEL SIZE:

SITE PLAN

Solar Power

CONTRACTOR

SUNWORKS 1030 WINDING CREEK RD, STE, 100 ROȘEVILLE, ÇA 95678 CSLB: #441690 / C-10, C-46 (916) 409-6900

ITY CIERTINY THAT THE WORK PROPOSED TO BE DONE ON IPANNIS IN COMPONANCE WITH ALL CODES AND ANCES OFTHE ALL OF CITY OF PERSON AND FURTHER, IF ICHG OR EPROMS ARE DISCOVERED, I UNDERSTAND THAT THE PERFORMED WILL DE REQUIRED I COMPLY WITH THE CODE ROSPANCES OF THE ANL OF CITY OF PRESEND PRICE TO THE AND THE CODE ROSPANCES OF THE ANL OF CITY OF PRESEND PRICE TO THE AND THE CODE TO THE CODE TO THE ANL OF CITY OF PRESEND PRICE TO THE CODE TO THE CODE TO THE ANL OF CITY OF PRESEND PRICE TO THE CODE TO THE TO THE CODE TO THE CODE TO THE CODE TO THE CODE TO THE CODE

#44 1890 / C-10, C-45

Property Owned & Managed by:

MVRP PV 14114 BUSINESS CENTER DR. MORENO VALLEY, CA 92553

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	3/29/2020	PLANSET	A.L.	-
	878/2020	RÉVERON 1	AL	-
	BT 9/2020	REVISION 2	AL	-
	10/14/2020	REVISION 3	A.L.	-
	18/20/2020	REVISION 4	AL	-
	11/5/2036	SITE PLAN	P.R.	
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SYST	EM INFO:		•	_

DESCRIPTION:

SITE PLAN

MVRP PV

14114 BUSINESS CENTER DR. **MORENO VALLEY, CA 92553**

PHOTOVOLTAIC GENERAL NOTES		SYSTEM INFORMATION	CITY APPROVAL STAMPS	(916) 409-6
ALL MATERIALS, EDMEMBERT, INSTALLATION AND WORK PERFORMED SHALL BE IN ADCORDANCE WITH THE FOLLOWING CODES:	THE SAME OF RETIRE PROTONOLING MODULES, PARILS OR SOLAR VOLTAK ROLL ROOFING MATERIAL SHALL HAVE THE SAME OF RETIRE USED PIRE-RESISTANCE RATING THAY THE BUILDING ROOF-CONTING MATERIAL	SYSTEM: SYSTEM AC CAC SIZE: 102,85 KM SYSTEM AC CAC SIZE: 94.49 KM		I HERBEY CERTIFY THAT THE WORK PROFOS THESE PLANE IS IN CONFORMANCE WITH ALL ORDERANCES OF THE ALL OF CITY OF FREE
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• 2019 CMC • 2019 CMC • 2019 CMC	33 EQUIPVENT CROUNDING CONDUCTOR FOR PV MCDULES SMALLER THAN 6 AND SHALL BE PROTECTED FROM PRYSHAL DUMMER BY A RACEMAY OR CHOKE APROR (CCC 690.46 & 284120(C))			SIGNATURE DATE
2019 BULDING PURROY ETROPICKY STANDARDS LE GOURREST STAND SEEDS BY A RECORDED TESTING LABORATURY AND INSTALLED FROM THE LEVEL SCHOOL PROPERTY AND LINE SEEDS AND SHOPLED BY A RESTRICTIONS. FOR THE VIGILATION AND LINES AND SHOPLED BY A RESTRICTIONS.	31 AVERAGE SINAR CONSUMPTION IS NOT TO EXCERC 120% OF AVERAGE ANNUAL CONSUMPTION			
LISTING REQUIREMENTS AND THE WAMERSCHRER'S RISTRUCTORS, ECC VIOLE(S)4(C), 6984(6), AND 69012(3). INSTRUCT REQUIREMENT OFTIS, STATUSTS, EVANUATE OUTLETS, VENTULATORS INTAKE ARE OFDING SHALL NOT BE CASHESS BY THE SQUAR PRODUCT, LOSS CHIEF	THE PROJECT SHALL COMPLY WITH ALL THE LATEST APPUCABLE NATIONS. ELECTRIC CODE (MEC) REQUIREMENTS [MEC ANTICLES AND AND 705], NED RECOMPLINES, STATE OF CAUPENIA REQUIREMENTS, BUILDING CODES, AND SHALL DEFINAL ELECTRICAL PREMITTIES FOR THE DOOR PLANT INSTALLATION.			
COMERED BY THE SOURS PHOTOMOLITAIN SYSTEM ALL QUIDDOR EDURPMENT SHALL BE NEWA 38 RATED, WILLIAMS ALL ROOF MOUNTED TRANSPIREN BOXES AND SWITCHES.	M MEMORING CLEARANCES AROUND THE EDISING ELECTRICAL ECOPPMENT AS WELL AS THE MEM ELECTRICAL SOLPMENT THAT BE MANUFAMED IN ACCORDANCE 13TH CEC 110 28.	SOORE OF WORK		
SMITCHES SILL EQUIPMENT SHALL BE PROPERLY GROUNDED AND BOADED IN ACCORDANCE WITH MEC ARTICLE 250	THE PHOTOTYCUTAGE AVERTER WILL BE LISTED AS UL 1741 COMPLIANT. (CET 690.4(B))	SCOPE OF WORK		
FIT SYSTEM DIC DROW! AND INNERTER OUTPUT CONDUCTORS AND EQUIPMENT SHALL BE PROTECTED AGAINST OWERCOMENT FEEL STORMAN.	37 ADEQUATE SPACING WAST BY WANTANED BETWEEN ANY PLUNDING SCIENT MENTS EXTENDING THE ROOF AND THE WHOTH FOR OF THE PHOTOMOLITAC PANELS (6 WINNIUM RECOMMENDED).	WSTALLNG:		
The property consideration of the provide controlled conductors that are note than 3 feet in Jerstin head. As allowed on note than 1 feet than 1 feet than 1 feet and 1 feet and 1 feet than 1 feet and 1 feet than 1 feet tha	LE PHOTOVOLTAGO OUTPUT ORICHITS OPERATING ABOVE 30 VOLTS 5-ALL 6E INCTACLED IN READILY ACCESSIBLE LOCATIONS AND TO ELECTRICAL RACEWAYS. [CCC 680.31 (A)]	(242) RODE WOUNTED PHOTOMOCHAIC WOOMLES (I) SOLAREDGE SEIGRIUS		
		SOLLEGA FASTRACK 10"		
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MEANS SHALL BE PROVIDED TO DISCOMMENT THE PY SYSTEM FROM ALL WRING SYSTEMS INCLUDING POWER SYSTEMS, EMPTY STORMED SYSTEMS, AND UTBLIZATION EQUIPMENT AND ITS ASSOCIATED FROM SESSIONAL OCC 690-13.	(LEC 1990.3)			
ALL COMMUNICIONS EXPOSED TO MEAT OR SHALL SO LISTED AND IDENTIFIED FOR USE IN INSECT SURLICHT [NEC 620.31(2) THROUGH (G), 310.16(0).]				
THE MODULES COMBINERIES MUST BE TIPE USE-2 OR USTED FOR PHOTOMOLTRIC (PV) WATE [NED 689.5N(C)]				Property Owned & Ma
ALL CONDUCTORS SHALL SE MARKED ON EACH END FOR UNKQUE DENTRICATION (MEC 580.31(8))				MVRP P
ALL CONDUCTIONS TO BE OF MATERIAL APPROVED BY THE CODE AND THER MISULATIONS TO BE RATED TO NOT LESS THAN 80°C BODYCLTS UNMIRED.				14114 BUSINESS C
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THE PRE-STRANDED CARKE CONNECTIONS MIGST BE MADE IN LINES AND TERMANALS LISTED AND MARKED FIRE USE, OSCI 110.14.				
LI CHURICOZI, (RUTINAL), COMUCCOTO SEMELATOR SPALL DE CALID METE, CRAV, CR WITH 5-WHITE STREES, CE 2014, 2014, 1-4 40-022. Mo. AL CHARACHER CONTUCTIONS SPALL DE OPRÀS MES WITHOUT COMERNI, CR WITH DOULTON OF DEZIA DE CRESS	ABBREVIATIONS	LEGAL DESCRIPTION	DRAWING INDEX	NO. DATE REVISIONS EL
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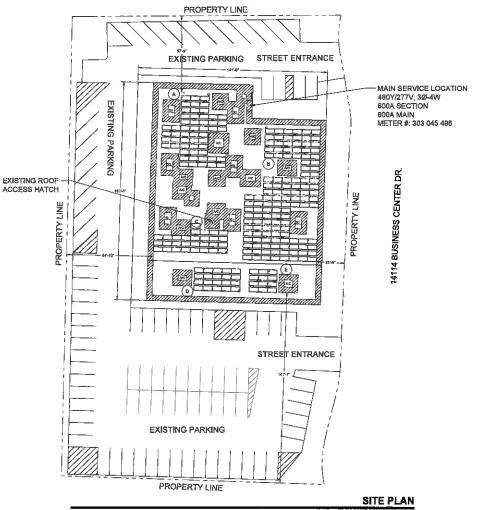
SUNWORKS 1030 WINDING CREEK RD, STE, 100 ROSEVILLE, CA 95676 CSLB: #441690 / C-10, C-46

#441696 / C-10, C-4J

ENTER DR. CA 92553

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YATEM INFO:			
	ZE: DC STC: 102.85		

AGE



SCALE: 3/64" = 1'-0"

		ARRAY INF	ORMATION		
ROOF	A	В	С	D	ε
ROOF PITCH	<5°	<5°	<5°	<5"	<5°
ROOF MATERIAL	ROLLED COMP.				
MODULE COUNT	54	127	29	20	12
MODULE AREA	1127.9SQ.FT.	2652.6SQ.FT.	605.7SQ.FT.	417.7SQ.FT.	250,68Q,FT.
ARRAY TILT	10°	10°	10°	10°	10°
ARRAY AZIMUTH	181°	181°	181°	181°	181°

SITE NOTES

ROOF ACCESS FOR BUILDINGS OTHER THAN ONE AND TWO DWELLING UNITS SHALL BE PREVIOUS WITH A WINNIUM OF SOL FOOT (8") WIDE CLEAR PERHETER AROUND THE EDGES OF THE ROOF.

- E) SOLAT PHOTOMOLTAIC POWER SYSTEMS SHALL BE WISTALLED IN ACCORDANCE WITH SCIENCE SCI.1.1 THROUGH \$63.1.2, THE CAUFTRINA BYLDING CODE. OR CAUFORNIA RESIDENTIAL, CODE, AND CAUFORNIA ELECTRICAL CODE.

 -FIRE CODE 603.11
- PATHNAYS SHALL BE OVER ABSAS CAPABLE OF SUPPORTING PREPIORIERS
 ACCESSING THE ROCE, PATHNAYS SHALL DE LOCATED IN AREAS WITH MENAL
 CRESTALETONS SHORL AS WENT PIPES, CONDUIT OR INCOMMENT, EQUIPMENT,
 -PRE CODE 1053.11.
- POPP ACCESS POWER SHALL BE CONTROL OF MINES HAVE OF OUT SOLUCIES OF PROPERTY OF THE PROPERTY O

THE NOTES: (CHAPTER & OF CAUFORNIA THE PODE)

605.11.3.31 ACCESS, THERE SHALL BE A MAHARIM SH PROFT-HIDE (1829 MAN) CLEAR PERMETER ANGUARD THE EDIES OF THE ROOM. ENCYTHING HERE DITHER ANS OF THE BULLHAME IS 230 FEET (TH 220 MAY) OR LISS, THERE SHALL BE A MAHARIM 4-FOOTT-HIDE (1200 MAY) CLEAR PROMISERS ANGUARD THE EDICKS OF THE ROOM.







CONTRACTOR

SUNWORKS 1030 WINDING CREEK RD. STE. 100 ROSEVII.LE, CA 95678 CSLB: #441690 / C-10, C-46 (916) 409-6900

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#441690 / C-10, C-46

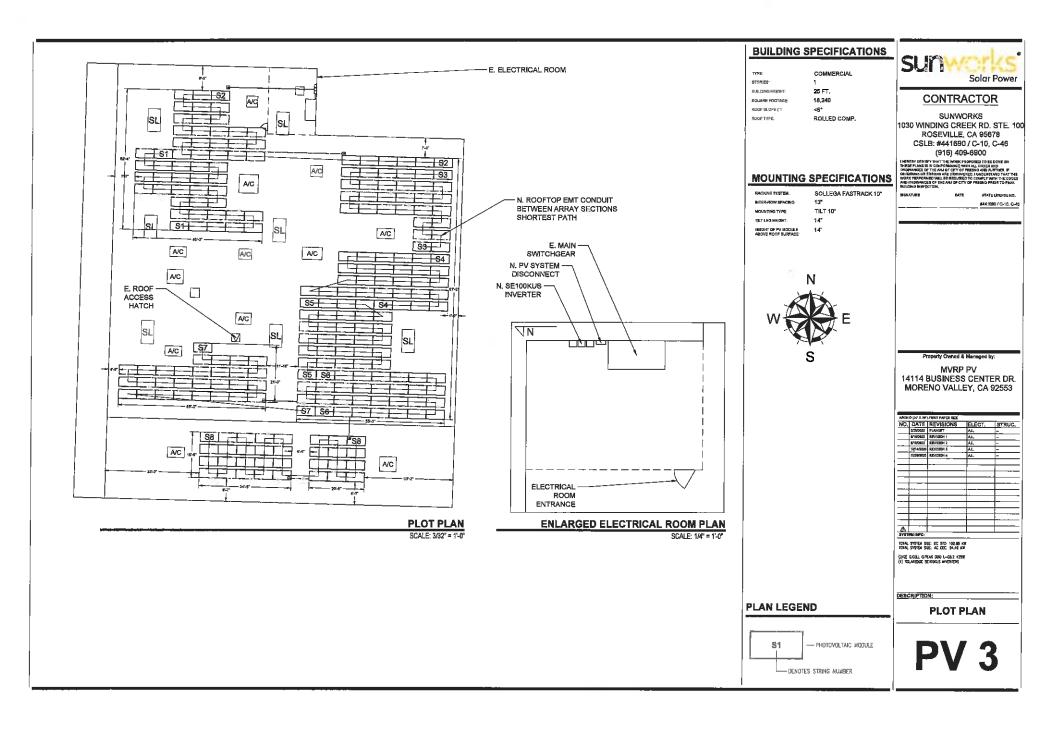
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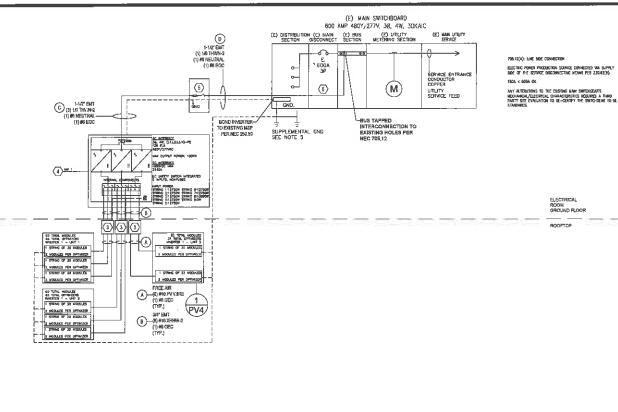
MVRP PV 14114 BUSINESS CENTER DR. MORENO VALLEY, CA 92553

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TOTAL	ELEIEN ZI	E: 8C STC: 102.85	ICW	

DESCRIPTION:

SITE PLAN





EQUIPMENT NOTES

PROTOROGING MODULES WICLIDE \$12 AND DISTOROGE NATED MC4
CONNECTIONS FOR MODULE INTERCONNECTION OF MOT REMOVE THE QUICK
CONNECTS, OTHERWISE THE MODILE MARRANTY AND THE UL USTING TRILL BE
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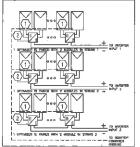
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MINESTERS SHAPEONIX OF CYPSEN PRODUCTION AT CHEMICADE UPON GROD
DISCONNECT. MOVING THE LUCET ON THE ALL RECORDINGT OF THE OPEN
POSITION WILL COST PANEL PRODUCTION FROM THE OF OPTIMIZER. SEE SPEC.
SHEET ON HYP

MODULE/OPTIMIZER STRING INVERTER 1-2





CONTRACTOR

SUNWORKS 1030 WINDING CREEK RD. STE. 100 ROSEVILLE, CA 95678 CSLB: #441890 / C-10, C-46 (916) 409-6900

I HEREBY CERTIFY THAT THE WORK FROM DED TO BE DONE ON THE SEE LANS 18 IN CORPORAMINE WITH ALL DODGS AND TO REQUIRE AND THE PART OF THE PAR

SIGNATURE

STATE (ICE/65E NO. #441890 / C-10, C-46



Property Owned & Managed by:

MVRP PV 14114 BUSINESS CENTER DR. MORENO VALLEY, CA 92553

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TOTAL SYSTEM SZE: DC STC: 102.65 KW TOTAL SYSTEM SZE: AC CEC: 94.49 KW (242) OCELL O.PEAK DUO L.-CEL2 428W (1) SOLAMEDUE SENDOUS WIESTERS

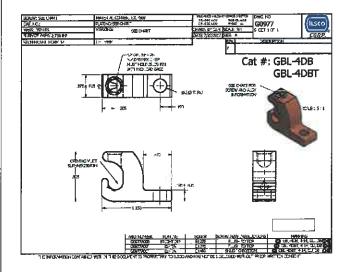
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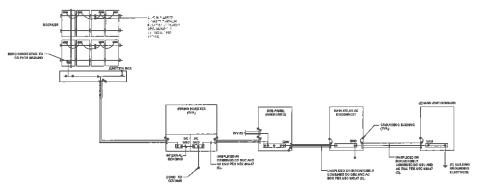
SINGLE LINE DIAGRAM

PV 4

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CONTRACTOR

SUNWORKS 1030 WINDING CREEK RD, STE, 100 ROSEVILLE, CA 95678 CSLB: #441690 / C-10, C-46 (916) 409-6900

I HERREY CERTIFY THAT THE MORK PROPOSED TO BE DONE ON THESE PLAYER IN LODGED AND COMPONENCE WITH ALL COCKES AND CORQUANCES OF THE ANAL OF THE PROPOSED ON LITTERS, IF DAUGNING OF THE ANAL OF CHISTOPE, IF DAUGNING ON THE PROPOSED AND DISCOVERED, I UNDERSTAND THAT THE WORK, PRAYORISED HILL BE REQUIRED TO CARRY WHITH THE COCKES AND CHISTOPE OF THE ANAL OF

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DATE

#441690 / C-10, C-46

Property Owned & Managed by:

MVRP PV 14114 BUSINESS CENTER DR. MORENO VALLEY, CA 92553

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TOTAL SYSTEM SIZE: BC STC 102.85 KW TOTAL SYSTEM SIZE: AC CEC: 94.49 KW (242) O.CELL G.PEMC DVO L-68.2 425W (1) SOLANDEGE SCHOOLS RYSKYEDS

DESCRIPTION:

GROUNDING

PV₅

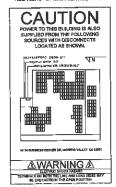




WARNING TURN OFF PHOTO: OLTAROAC DISCONNECT PRIOR TO WORKING BYGIDE PANEL

2 BUILDING / STRUCTURE

NEC 705.10 - BY MAIN SERVICE PANEL:

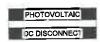


3 DC DISCONNECT/ BREAKER/ RECOMBINER BOX

WARNING

PV SYSTEM LABELING REQUIREMENTS NOT TO SCALE

COLTAGE IS ALWAYS PRESENT EXPOSED TO SUNLIGHT



PV INVERTER DC DISCONNECT

POWER SOURCE OUTPUT

MAX SYSTEM VOLTAGE: MAX CIRCUIT CURRENT: MAX RATED OUTPUT CURRENT OF THE CHARGE CONTROLLER OR DC-TO-DC CONVERTER (IF INSTALLED)

EMT/ CONDUIT RACEWAYS

SOLAR PV DC CIRCUIT

PHOTOVOLTAIC POWER SOURCE

(5) INVERTER

8 BREAKER PANEL/ PULL BOXES



AC PHOTOVOLTAIC DISCONIE

WARNING BLEUTRICAL SHOCK HAZARD TERMINALS ON BOTH THE LIME AND LOAD SIDE MAY BE ENERGIZED IN THE OPEN POSITION

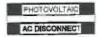
WARNING TURN OFF PHOTO FOLTAIC AC DIBCONNECT PRIOR TO WORKING INSIDE PANEL

AC PHOTOVOLTAIC DISCONNECT

(6) PRODUCTION NET METER

WARNING DUAL POWER SOURCE SECOND SOURCE IS PHOTOVOLTAIC SYSTEM

7 AC DISCONNECT/ BREAKER/ POINTS OF CONNECTION



THOTOVOLTAIC DISCONST

WARNING THIS EQUIPMENT FED BY MULTIPLE SOURCER, TOTAL RATING OF ALL OVERCURRENT DEVICED, EXCLUDING MAIN SUPPLY OVERCENT DEVICE, SHALL NOT EXCEED AMPACITY OF BUSBAR

WARNING TERMINALS ON SOTH THE LINE AND LOAD SIDE MAY BE ENERGIZED IN THE OPEN POSITION

SIGNAGE REQUIREMENTS

GENERAL FIRE GUIDELINES &

MARKING REQTS:

SEC. 4 MANDRINGE LABELS AND YMPROBING BEGUS.

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- 5. MAY SERVE ESCONDECT.

 1. ESSERVE ESCONDECT

- COMMITTEE LETTERING ON A RED BACKGROUND.

 (2) MALL LETTERS SMALL BE CAPITALIZED.

 (4) ANUL OR SWALM FORT, MON-BOLD.

 AUREOUS.

MAINDEL: (1) REPLECTINE, WEATHER RESISTANT MATERIAL, SUTHELE FOR THE CHARGOLIMENT (USE UL -- 989 AS STANGARD FOR MEATHER RATHE), OURSELE ADMESSA'S MATERIALS MEET THIS

- ROUNDEND. COMPANY, PARTIES, COMPANY, DALIESMAN, DALIESM

MATERIALS USED FOR MARRING SMALL BE REFLECTIVE, MEATHER RESISTANT, AND SUTTRILE FOR THE ENVIRONMENT, ALL LARRES SMALL BE WARE LETERS OF RED BACKGROUND.

THE MARRINGS SHALL BE OF SUPPOSENT DURABULTY TO WITHSTAND THE ENVIRONMENT BY BUILDED.

MED. 110.21

Solar Power

CONTRACTOR

SUNWORKS 1030 WINDING CREEK RD, STE. 100 ROSEVILLE, CA 95678 CSLB; #441690 / C-10, C-46 (916) 409-6900

KOMATURE	DATE	STATE LICENSE NO.
		#441680 / C-10, C-46

roperty Owned & Managed by:

MVRP PV 14114 BUSINESS CENTER DR. MORENO VALLEY, CA 92553

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(242) DICELL DEFAK THIS L-CILZ 425W (1) SCLAREDGE SELDORUM MINERTERS

DESCRIPTION:

SIGNAGE

MAIN SERVICE DISCONNECT

WARNING TERMINALS ON BOTH THE UNE AND LOAD BIDE MAY BE ENERGIZED IN THE OPEN POSITION

WARNING

WARNING DUAL POWER SOURCE

CAUTION IOTOVOLTAIC SYSTEM CIRCUIT IS BACKFED



BY RAPID SHUTDOWN SWITCH (10) BY RAPID SHU (WITHIN 3 FT)

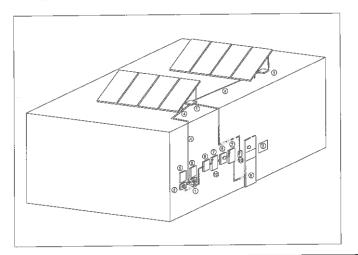
> (680.50,C)(1)(a) YELLOW WITH BLACK LETTERING) SOLAR PV SYSTEM EQUIPPED WITH RAPID SHUTDOWN

TURN RAPID SHUTDOWN SMITCH TO THE "OFF" POSITION TO SHUT DOWN PV SYSTEM AND REDUCE SHOCK HAZARD IN ARRAY



RAPID SHUTDOWN SWITCH FOR SOLAR PV SYSTEM

*RAPID SHUTDOWN SWITCH CAN BE EITHER THE AC DISCONNECT SWITCH OR A SEPARATE SWITCH, SEE PV 4 FOR TYPE OF RS SWITCH





State dept depth states &

Three Phase Inverter with Synergy Technology

for the 277/480V Grid for North America

SE66.6KU5 / SE100KUS



and thaily designed to wook with passer operations

- busy two-paracon installation each with mounted separately, enalogied with cables for sinuse connection between traits
- Independent operation of such un terrables higher welfare and every serviceability
- No readed ground hear woll/rell mounted, or harbonistly request one or the rose of as [10] "exclinations."

Integrated are brule protection and rapid shadetown for NEC 2011 and 2017, per orticle 893.1; and 690.12

- Fixed voltage liverar for superior efficiency (90.5%) and larger strings
- Setumpated DC Setally Cyclick
- # Built-in RS485 Surge Protection, to bette valuestand lightning events

solaredge

solaredge

/Three Phase Inverter with Synergy Technology for the 277/48/19 offit for Navia America

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Power Optimizer For North America

INVERTERS



POWER OPTIMIZER

First years and applications and the macefulin force). "You come much affective salicities for examposite and house field installment without

- Spec (vally designer to work with Solarite go inventors Advanced maintenance with module level
- ↓ Lp to 25% mare energy
- / Superior efficiency (99 5%)
- Balance of System cost reduction: 50% lies. cab os, Futer area combiner series, over 2% unger string lengths possible.

P860

- / Past Installation with a single point
- Use with two PV modules connected in parallel

Module-leve voltage shutdown for instal or and firefighter safety

/ Power Optimizer For North America

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SUNWORK\$ 1030 WINDING CREEK RD, STE. 100 ROSEVILLE, CA 95678 CSLB: #441690 / C-10, C-46 (916) 409-6900

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URE	DATE	STATE LICENSE NO.
		#441890 / C-10, C-4

Property Owned & Managed by:

MVRP PV 14114 BUSINESS CENTER DR. MORENO VALLEY, CA 92553

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TOTAL SYSTEM SIZE: DC STC: "UZ-85 RW TOTAL SYSTEM SIZE: AC OSC: 94-69 KW (242) QCCLL GLPEAK DJO .-GB2 425W (1) SOLANEDGE SEIOMUS BYFRIERS

DESCRIPTION:

EQUIPMENT SPECIFICATIONS

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The Simple Solar Racking Solution™



The Simple Solar Racking Solution™

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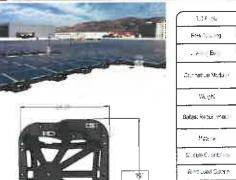
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FastRack510™

The simple solar racking solution for flat roof or ground mount PV installations.

- 5° or 10° mounting solution
- Eimple, mindolar, one pri de design
- Universal de ¿gn compatible valh all framed modules.
- Fully ballested , heat welded, anchored and in find operans.
- Roof friendly with round edges and four point loads
- One size brit with all top down connections.

Easy to install:

Position FastRacks

2 Add Ballast and Clamps

Attach Modules

Injection multipop UV rated Nylvin6

UL 437 Integral of grounding

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Pill stamp enginering and ballast layout.

UL 2713 Class "A" Tijne 1, 2, 3 Hodules









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Warranty

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FastRack510™

(916) 409-6900

CONTRACTOR

SUNWORKS

1030 WINDING CREEK RD, STE, 100

ROSEVILLE, CA 95678 CSLB: #441690 / C-10, C-46

Solar Power

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#441890 / C-10. C-44

Property Owned & Managed by:

MVRP PV 14114 BUSINESS CENTER DR. MORENO VALLEY, CA 92553

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SYSTEM INFO:					

TOTAL SYSTEM SZE: DC STC: TOZJES KW TOTAL SYSTEM SZE: AC CEC: 94,49 KW

DESCRIPTION:



T: 2K Estate Automor, January Contigued NY 19346 Telephone: 407 755-8516

Subject: ETI, Evaluation of ScienEdge Products to NEC 2017 Rapid Shutdown Requirements

This letter represents the testing results of the below listed products to the requirements contained in

National Electric Code, 2017, Section 690,12 requirement for rapid shutdown.

UL 1741, UL 1741 CRD for rapid shutdown

The evaluation was done on the PV Rapid Shutdown System (PVRSS), and covers installations consisting of entirebers and inverters with part numbers listed below.

The testing done has verified that controlled conductors are limited to:

- Not more then 30 volts and 240 voltampens within 30 seconds of rapid shutdown initiation ourcide the array.
- Not more Lian 80 voits and 240 voltamperes within 30 seconds of rapid shutdown initiation. inside the array.

The rapid shundown initiation is performed by either disconnecting the AE feed to the inverter, or -if the inverter DC Safety switch is readily accessible - by turning off the DC Safety switch.

Version, 8-September-2018

PB to lowes by 901 to 350; followed by -ACB or -TFI. OP followed by 001 to 900; followed by -LV, -MV, -IV or -EV P followed by 001 to 850.

SP followed by 001 to 350.

*When potimizers are connected to 2 or more modules in series, the maxingut voltage may exceed 80%. Following the Implementation of the NEC 2017 rapid shutdown of 80% may inside of the array of the beginning of 2019, modules exceeding this

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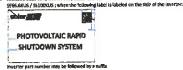
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Agdress, Introdel 3933 US-11 Cortland NY 13046 Talaphone 907-768-6519

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If there are any questions regarding the results contained in this report, or any of the other services offered by intertek, please do not heritate to contact the undersign

intertek

Astronous, Interferis 3990 US 11 Cardiand MV 1304

Please note, this Letter Report does not represent authorization for the use of any Intertek certification

Howard Llu Equipmenting Team, Leader

" WI more

6/21/2018

STATE LICENSE NO. #441889 / C-10, C-49

CONTRACTOR

SUNWORKS

1030 WINDING CREEK RD. STE. 100

ROSEVILLE, CA 95678

CSLB: #441690 / C-10, C-46

(916) 409-6900

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Property Owned & Managed by:

MVRP PV 14114 BUSINESS CENTER DR. MORENO VALLEY, CA 92553

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TOTAL SYSTEM SIZE: DC STC: 102.65 KW TOTAL SYSTEM SIZE: AC CCC: 94.49 KW (242) QDEL OPEAK DIO L-GB2 425W (1) SOLANDIE SZIDÓRUS INVERTIONS

DESCRIPTION;

UL LISTING

Page 2 of 3

Version: 8-September-2015

62018 Intertek GFT-OP-10a

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intertek

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Test Verification of Conformity

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Certificate of Compliance

Master Contracts 254141 70167867

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The products listed below are eligible to bear the CSA Mark shown with adjacent indicators 'C' and 'US' for Canada and US or with adjacent indicator 'US' for US only or without either indicator for Canada only.



Jamed by: Keeming (Simon) Shen-Xueming (Simon) Shen

CLASS - C33: 110 - POWER SUPPLIES-Photovolule Modules and Proofs CLASS - C33: 190 - POWER SUPPLIES-Photovolule Modules and Power - Certified to US Saundards

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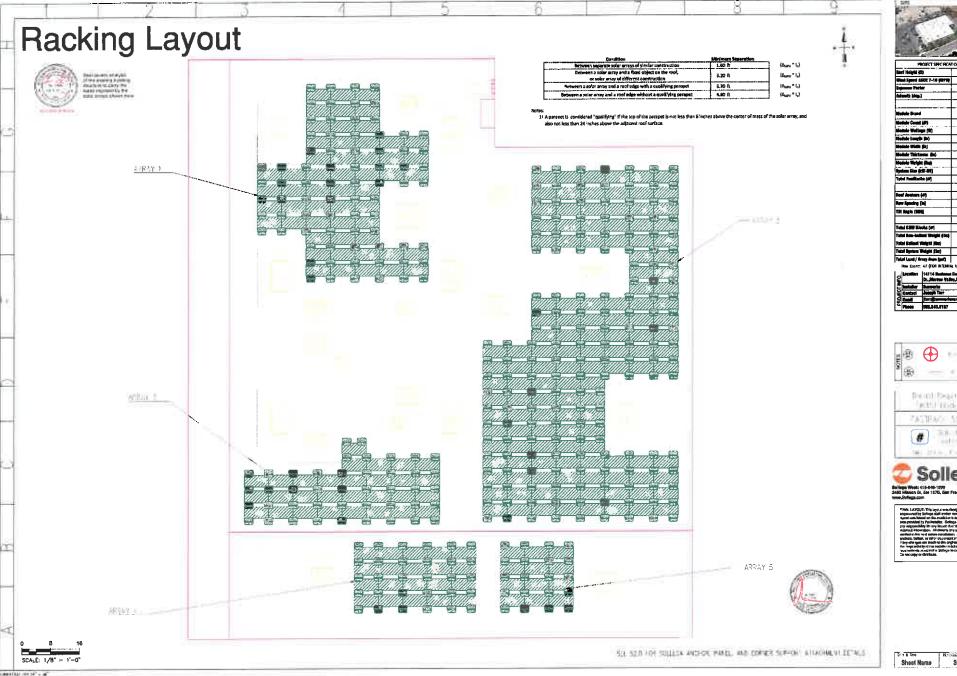
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(14-14) هار هطاراً	184,81
Tyled Pasidianiles (49)	334
fixed Assistant (47)	
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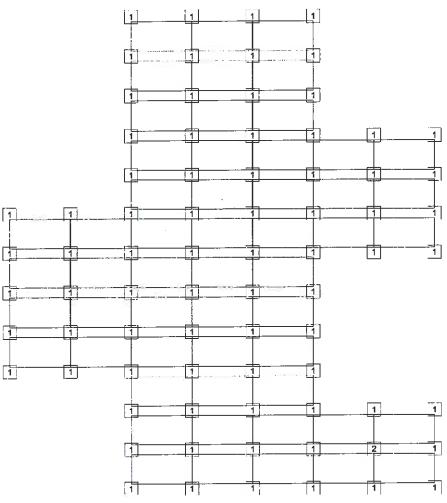




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Ballast Layout





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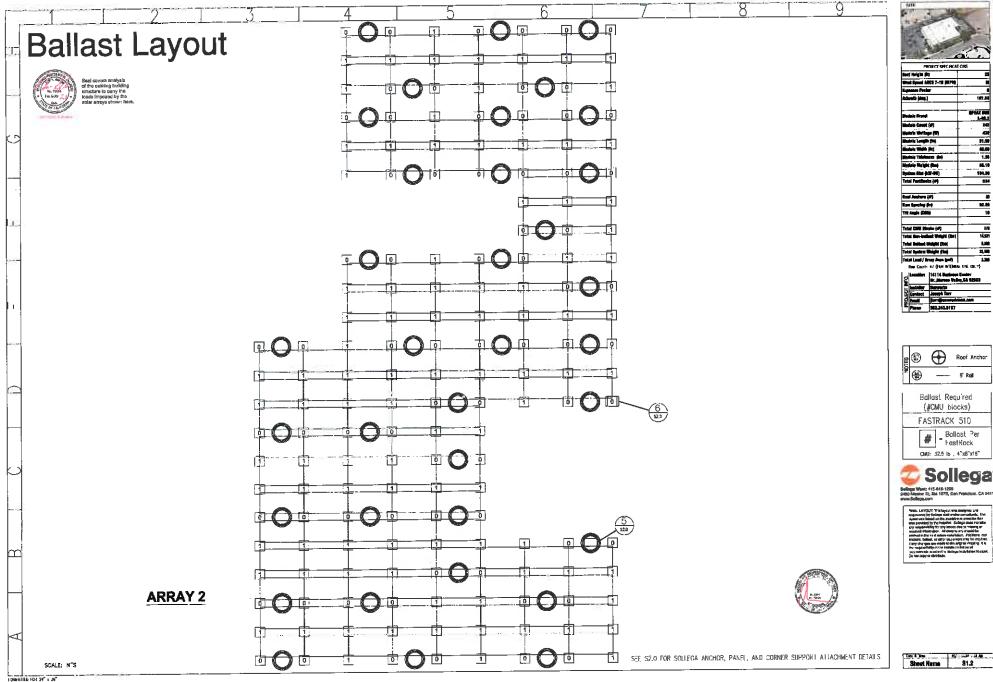




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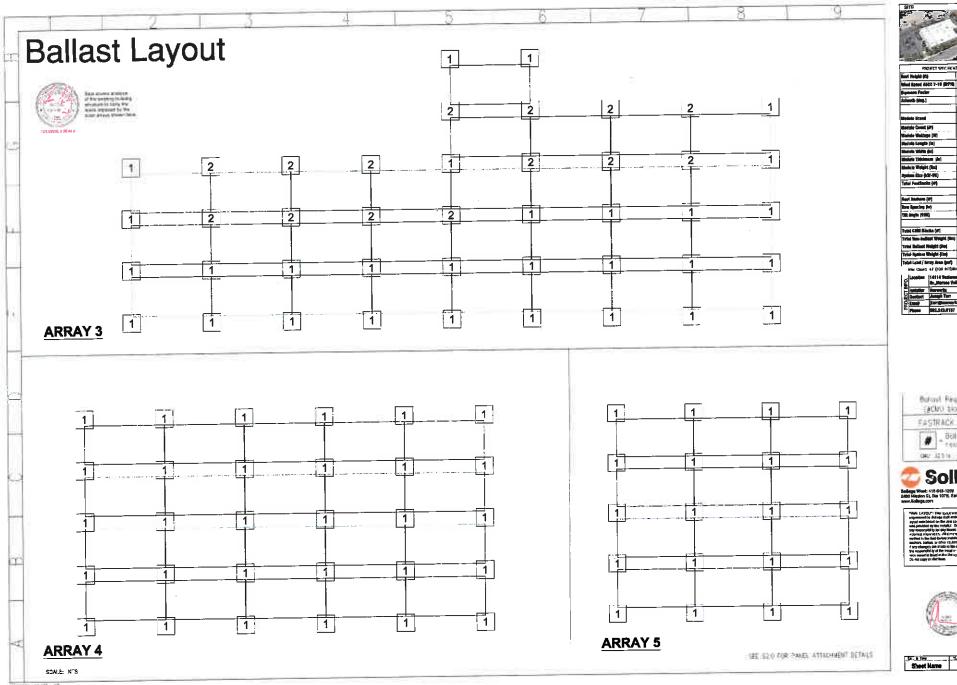




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PROJECT SPEC FICAL	I-ONS
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Madela William (in)	40.51
Modelo Titalmass (in)	1.31
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Tetal Cilil Blacks (#)	tu tu
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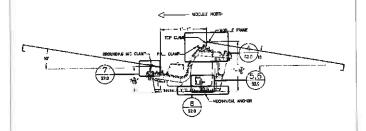




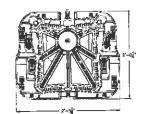


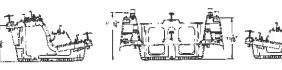
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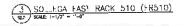


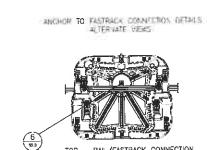




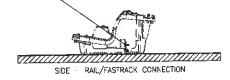




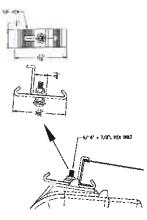




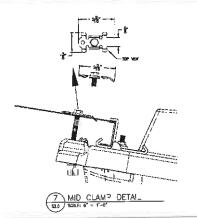
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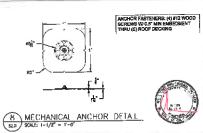


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STREAM PROPERTY STEEL



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NOTICE OF PUBLIC HEARING RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact ALUC Planner Paul Rull at (951) 955-6893. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The City of Moreno Valley Community Development Department should be contacted on non-ALUC issues. For more information please contact City of Moreno Valley contact Mr. Jesus Huerta at (951) 413-3234.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to prescription or prescription or prescri

PLACE OF HEARING: Riverside County Administration Center

4080 Lemon Street, 1st Floor Board Chambers

Riverside California

DATE OF HEARING: May 13, 2021

TIME OF HEARING: 9:30 A.M.

Pursuant to Executive Order N-25-20, this meeting will be conducted by teleconference and at the Place of Hearing, as listed above. Public access to the meeting location will be allowed, but limited to comply with the Executive Order. Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org

CASE DESCRIPTION:

<u>ZAP1458MA21 – MVRP PV (Representative: Sunworks Inc.)</u> – City of Moreno Valley Case No. PEN20-0198 (Plot Plan), a proposal to construct a 5,588 square foot rooftop solar panel system on an existing industrial building on 1.62 acres located at 14114 Business Center Drive, northerly of Golden Crest Drive, westerly of Elsworth Street, and southerly of Alessandro Boulevard (Airport Compatibility Zone C1 of the March Air Reserve Base/Inland Port Airport Influence Area).



RIVERSIDE COUNTY **AIRPORT LAND USE COMMISSION**

APPLI	CATION FOR MAJOR LAND US	E ACTION REVIEW
ALUC CASE NUMBER	ZAP 1458 MAZ DATE SUBMITTED:	34-21
APPLICANT / REPRESENT	FATIVE / PROPERTY OWNER CONTACT INFORMATION	
Applicant	Sunworks Inc.	Phone Number (916) 409-6900
Mailing Address	1030 Winding Creek Road #100	Email jtarr@sunworksusa.com
	Roseville, CA 95678	
Representative	Alexander Martinez	Phone Number 626-716-2916
Mailing Address	990 S. Arroyo Parkway #4	Email projectmanager@cfa-la.com
	Pasadena, CA 91105	
Property Owner	MVRP PV	Phone Number (
Mailing Address	14114 Business Center Drive	Email
	Moreno Valley, CA 92553	
LOCAL JURISDICTION AG	ENCY	
Local Agency Name	Moreno Valley Community Development	Phone Number (951) 413-3234
Staff Contact	Jesus Huerta	Email jesush@moval.org
Mailing Address	14177 Frederick Street	Case Type
	Moreno Valley, CA 92553	General Plan / Specific Plan Amendment
		Zoning Ordinance Amendment Subdivision Parcel Map / Tentative Tract
Local Agency Project No	PEN20-0198	Use Permit
		Site Plan Review/Plot Plan Other
		Other
PROJECT LOCATION		
Attach on accurately scaled n	nap showing the relationship of the project site to the airport boundary and runways	
Street Address	14114 Business Center drive	
	Moreno Valley, CA 92553	
Assessor's Parcel No.	297-210-026	Gross Parcel Size 1.62 acres
Subdivision Name	POM 21626	Nearest Airport and distance from Air-
Lot Number	6	port MarchAFB 1700
PROJECT DESCRIPTION If applicable, attach a detaile tional project description dat		
Existing Land Use	The existing land use is a 18,240 sqft eisting or	ffice space with an existing parkin
(describe)	Please see attached	

Proposed Land Use				
(describe)	The proposed scope	of work is to add solar pannels	to the existing building.	
<u> </u>			N/A	
For Residential Uses For Other Land Uses Kours of Operation N/A Number of People on Site Method of Calculation Height Data Site Elevation (above mean sea level) Height Hazards Does the project involve any characteristics which could create confusing lights, glare, smoke, or other electrical or visual hazar If yes, describe Number of People on Site (exclude secondary units) N/A Maximum Number 30 Based on C of O Height Data Site Elevation (above mean sea level) Height Hazards Does the project involve any characteristics which could create confusing lights, glare, smoke, or other electrical or visual hazar If yes, describe According to glare study	NI/A		IV/A	
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	•	Based on C of O and	occupancies for existi	ng office building
For Residential Uses For Other Land Uses (See Appendix C) Number of People on Site Method of Calculation Method of Calculation Now mean sea level Height Data Site Elevation (above mean sea level) Height of buildings or structures (from the ground) Flight Hazards Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight? No N/A N/A N/A NAMINUM Number of Parcels or Units on Site (exclude secondary units) N/A Naximum Number 300 Based on C of O and occupancies for existing office 1561 30 Flight Hazards Does the project involve any characteristics which could create electrical interference, confusing lights, glare, smoke, or other electrical or visual hazards to aircraft flight? No	ft.			
_	Height of buildings or structu	ures (from the ground)	30	ft.
Flight Hazards	Does the project involve any confusing lights, glare, smok	characteristics which could create electric e, or other electrical or visual hazards to a	,	
				d solar pannels
	pro	oduice any "green" or "yellow" g	lare. Please see attached	<u> </u>

- A. NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME: Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:
 - 1..... Completed ALUC Application Form
 - 1..... ALUC fee payment
 - 1..... Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
 - 1..... Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
 - 1..... CD with digital files of the plans (pdf)
 - 1..... Vicinity Map (8.5x11)
 - 1..... Detailed project description
 - 1..... Local jurisdiction project transmittal
 - 3. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
 - 3. Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. (Only required if the project is scheduled for a public hearing Commission meeting)

COUNTY OF RIVERSIDE AIRPORT LAND USE COMMISSION

STAFF REPORT

AGENDA ITEM:

3.5

HEARING DATE:

May 13, 2021

CASE NUMBER:

ZAP1462MA21 – Nautilus Abrasives Blasting and Powder

Coating, LLC (Representative: Renova Energy)

APPROVING JURISDICTION:

City of Moreno Valley

JURISDICTION CASE NO:

BAP21-0330 (Building Permit)

LAND USE PLAN:

2014 March Air Reserve Base/Inland Port Airport Land Use

Compatibility Plan

Airport Influence Area:

March Air Reserve Base

Land Use Policy:

Zones B1-APZ-I and B2

Noise Levels:

65 - 70 CNEL contour

MAJOR ISSUES:

None

RECOMMENDATION: Staff recommends that the Commission <u>CONTINUE</u> the matter to the June 10, 2021 meeting, pending completion of the Air Force review of the project.

PROJECT DESCRIPTION: A proposal to construct a 7,347 square foot rooftop solar panel system on an existing industrial building on 1.0 acre.

PROJECT LOCATION: The site is at 14451 Commerce Center Drive, northerly of Cactus Avenue, easterly of Old 215 Frontage Road, and southerly of Alessandro Boulevard, in the City of Moreno Valley, approximately 4,350 feet northwesterly of the northerly end of Runway 14-32 at March Air Reserve Base.

BACKGROUND:

Non-Residential Land Use Intensity: Pursuant to the Airport Land Use Compatibility Plan for the March Air Reserve Base/Inland Port Airport, the site is located within Compatibility Zones B1-APZ-I and B2, which limits average intensity to 25 people per acre and 100 people per single acre in Zone B1-APZ-I, and limits average intensity to 100 people per acre and 250 people per single acre in Zone B2. The proposed rooftop solar panels will not generate any occupancy.

Staff Report Page 2 of 6

March Air Reserve Base/United States Air Force Input: Given that the project site is located in Zones B1-APZ-I and B2 northwesterly of the northerly runway at March Air Reserve Base, the March Air Reserve Base staff was notified of the proposal to add rooftop solar panels, and sent a solar glare hazard analysis study for their review. As of the time this staff report was prepared, no comments have been received from the Air Force regarding this project.

<u>Flight Hazard Issues</u>: Structure height, electrical interference, and reflectivity/glare are among the issues that solar panels in the airport influence area must address. The project's 7,347 square feet square foot photovoltaic (PV) panel structures would be located on the rooftop of the existing industrial building within Compatibility Zones B1-APZ-I and B2.

Glint and Glare/Reflectivity

Based on the Federal Aviation Administration's Interim Policy for Review of Solar Energy System Projects on Federally Obligated Airports, no glare potential or low potential for temporary afterimage ("green" level) are acceptable levels of glare on final approach (within 2 miles from end of runway) for solar facilities located on airport property. However, potential for temporary afterimage" ("yellow" level) and potential for permanent eye damage ("red" level) are not acceptable levels of glare on final approach. No glare is permitted at air traffic control towers.

The project proposes 7,347 square feet of solar panels on the existing building rooftop with a fixed tilt of 5 degrees with no rotation, and an orientation of 180 degrees. The applicant has submitted a glare analysis utilizing the web-based Forge Solar, a copy of which is attached hereto. The analysis was based on a 2 mile straight in approach (as per FAA Interim Policy standards) to runways 14 and 32, and also based on the traffic patterns as identified by March Air Reserve Base staff (Runway 12/30 General Aviation, Runway 14/32 General Aviation, Runway 14/32 C-17/KC-135, Runway 14/32 Overhead). The analysis utilized a glide slope approach of 5.0 degrees. No glare would affect the Air Traffic Control Tower.

The analysis concluded that no glare would occur on the 2 mile approach to runways 14 and 32. However, some potential for glare was identified within the Air Force traffic pattern. Evaluation of the Air Force traffic patterns indicates that the panels would result in low potential for temporary after-image ("green" level glare). All times are in standard time.

Runway 14/32 C-17/KC-135 traffic pattern:

- Runway 32 Crosswind portion, totaling 36 minutes of "green" level glare, lasting up to 3 minutes a day, in February to March, and October to November from 6:00 a.m. to 7:00 a.m.
- Runway 32 Upwind portion, totaling 2,456 minutes of "green" level glare, lasting up to 15 minutes a day, between September to April from 7:00 a.m. to 8:00 a.m.

Runway 12/30 General Aviation traffic pattern:

• Runway 30 Crosswind portion, totaling 310 minutes of "green" level glare, lasting up to 10 minutes a day, in April to May, and August to September from 4:00 p.m. to 5:00 p.m.

• Runway 30 Downwind portion, totaling 1,888 minutes of "green" level glare, lasting up to 15 minutes a day, between April to October from 5:00 p.m. to 6:00 p.m.

Runway 14/32 General Aviation traffic pattern:

• Runway 32 Crosswind portion, totaling 2,942 minutes of "green" level glare, lasting up to 50 minutes a day, between May to August, from 6:00 a.m. to 7:00 a.m.

The total of 7,632 minutes of "green" level glare represents less than 3 percent of total day light time.

Electrical and Communication Interference

The applicant has indicated that they do not plan to utilize equipment that would interfere with aircraft communications. The PV panels themselves present little risk of interfering with radar transmission due to their low profiles. In addition, solar panels do not emit electromagnetic waves over distances that could interfere with radar signal transmissions, and any electrical facilities that do carry concentrated current will be buried beneath the ground and away from any signal transmission. There are no radar transmission or receiving facilities within the site.

<u>Prohibited and Discouraged Uses:</u> Glare from solar panels could potentially constitute a hazard to flight. However, based on the solar glare hazard analysis provided, the glare experienced would result in a low potential for temporary after-image ("green" level) which has been determined by the Federal Aviation Administration (FAA) to be an acceptable level for solar facilities on airports. Therefore, the hazard potential is low. Staff has included conditions to remedy unanticipated situations.

<u>Noise:</u> The March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan depicts the site within the 65-70 CNEL range from aircraft noise. The proposed solar panels are a non-noise sensitive use, therefore no mitigation measures are necessary.

<u>Part 77</u>: The elevation of Runway 14-32 at its northerly terminus is 1,535 feet above mean sea level (1,535 feet AMSL). At a distance of approximately 4,350 feet from the runway to the site, Federal Aviation Administration (FAA) review would be required for any structures with top of roof elevation exceeding 1,579 feet AMSL. The site's finished floor elevation is 1,555 feet AMSL and the existing building height is 20 feet. The establishment of the proposed rooftop solar panels would increase the maximum elevation by 1 foot, for a top point elevation of 1,576 feet AMSL. Therefore, review by the FAA Obstruction Evaluation Service is not required.

Open Area: None of the Compatibility Zones for the March Air Reserve Base/Inland Port ALUCP require open area specifically.

CONDITIONS:

1. Any outdoor lighting installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.

- 2. The following uses/activities are not included in the proposed project and shall be prohibited at this site.
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight or circling climb following takeoff or toward an aircraft engaged in a straight or circling final approach toward a landing at an airport, other than a DoD or FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport to the extent as to result in a potential for temporary after-image greater than the low ("green") level.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, congregate care facilities, hotels/motels, places of assembly (including, but not limited to places of worship and theaters), buildings with more than 2 aboveground habitable floors, hazardous materials and critical community infrastructure facilities.
 - (f) Highly noise-sensitive outdoor nonresidential uses. Examples of noise-sensitive outdoor nonresidential uses that are prohibited include, but are not limited to, major spectator-oriented sports stadiums, amphitheaters, concert halls and drive-in theaters.
 - (g) Any other uses not permitted in (Accident Potential Zone I/ Accident Potential Zone II) pursuant to DoDI 4165.57 Appx.2, Tbl.1.
 - (h) Hazards to Flight.
- 3. Prior to issuance of building permits, the landowner shall convey an avigation easement to the March Inland Port Airport Authority or its successor in interest, or provide evidence that such easement has been previously conveyed. The Airport Authority may waive this requirement in the event that the Authority determines that pre-existing avigation easements

- dedicated to the United States of America are sufficient to address its needs. Contact the March Joint Powers Authority at (951) 656-7000 for additional information.
- 4. March Air Reserve Base must be notified of any land use having an electromagnetic radiation component to assess whether a potential conflict with Air Base radio communications could result. Sources of electromagnetic radiation include radio wave transmission in conjunction with remote equipment inclusive of irrigation controllers, access gates, etc.
- 5. The maximum height of the building, including all roof-mounted equipment including solar panels, if any, shall be limited to 21 feet, and the maximum top point elevation shall not exceed 1,579 feet above mean sea level unless a "Determination of No Hazard to Air Navigation" letter authorizing a higher top point elevation has been issued by the Federal Aviation Administration Obstruction Evaluation Service.
- 6. All solar arrays installed on the project site shall consist of smooth glass photovoltaic solar panels without anti-reflective coating, a fixed tilt of 5 degrees and orientation of 180 degrees. Solar panels shall be limited to a total of 7,347 square feet, and the locations and coordinates shall be as specified in the glare study. Any deviation from these specifications (other than reduction in square footage of panels), including change in orientation, shall require a new solar glare analysis to ensure that the amended project does not result in any glare impacting the air traffic control tower or creation of any "yellow" or "red" level glare in the flight paths, and shall require a new hearing by the Airport Land Use Commission.
- In the event that any glint, glare, or flash affecting the safety of air navigation occurs as a result of project operation, upon notification to the airport operator of an event, the airport operator shall notify the project operator in writing. Within 30 days of written notice, the project operator shall be required to promptly take all measures necessary to eliminate such glint, glare, or flash. An "event" includes any situation that results in an accident, incident, "near-miss," or specific safety complaint regarding an in-flight experience to the airport operator or to federal, state, or county authorities responsible for the safety of air navigation. The project operator shall work with the airport operator to prevent recurrence of the incidence. Suggested measures may include, but are not limited to, changing the orientation and/or tilt of the source, covering the source at the time of day when events of glare occur, or wholly removing the source to diminish or eliminate the source of the glint, glare, or flash. For each such event made known to the project operator, the necessary remediation shall only be considered to have been fulfilled when the airport operator states in writing that the situation has been remediated to the airport operator's satisfaction.
- 8. In the event that any electrical interference affecting the safety of air navigation occurs as a result of project operation, upon notification to the airport operator of an event, the airport operator shall notify the project operator in writing. Within 30 days of written notice, the project operator shall be required to promptly take all measures necessary to eliminate such interference. An "event" includes any situation that results in an accident, incident, "nearmiss," report by airport personnel, or specific safety complaint to the airport operator or to

Staff Report Page 6 of 6

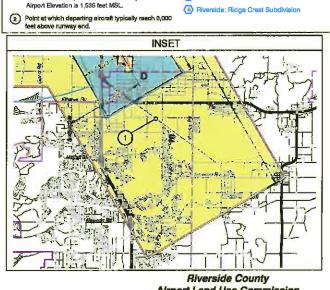
federal, state, or county authorities responsible for the safety of air navigation. The project operator shall work with the airport operator to prevent recurrence of the event. For each such event made known to the project operator, the necessary remediation shall only be considered to have been fulfilled when the airport operator states in writing that the situation has been remediated to the airport operator's satisfaction.

Y:\AIRPORT CASE FILES\March\ZAP1462MA21\ZAP1462MA21sr.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances [can vary from person to person. You may wish to consider what airport annoyances], if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b)

March JPA: Ben Clark Training Center



LEGEND

Compatibility Zones

Zone B1

Zone B2 Zone C1

Zone C2

Zona D Zone E

Zone M High Terrain Zone

All dimensions are measured from

Base map source: County of Riverside 2013

MILES

runway ends and centerlines.

Surface Limits FAR Part 77 Notification Area

Point at which aircraft on Runway 32 ILS approach descend below 3,000 feet above runway end.

AN BERNARDINO COUNTY

PERRIS RESERVOIR

SEE INSET AT RIGHT

Prepared by Mead & Hunt, Inc. (June 2013)

Airport Land Use Commission

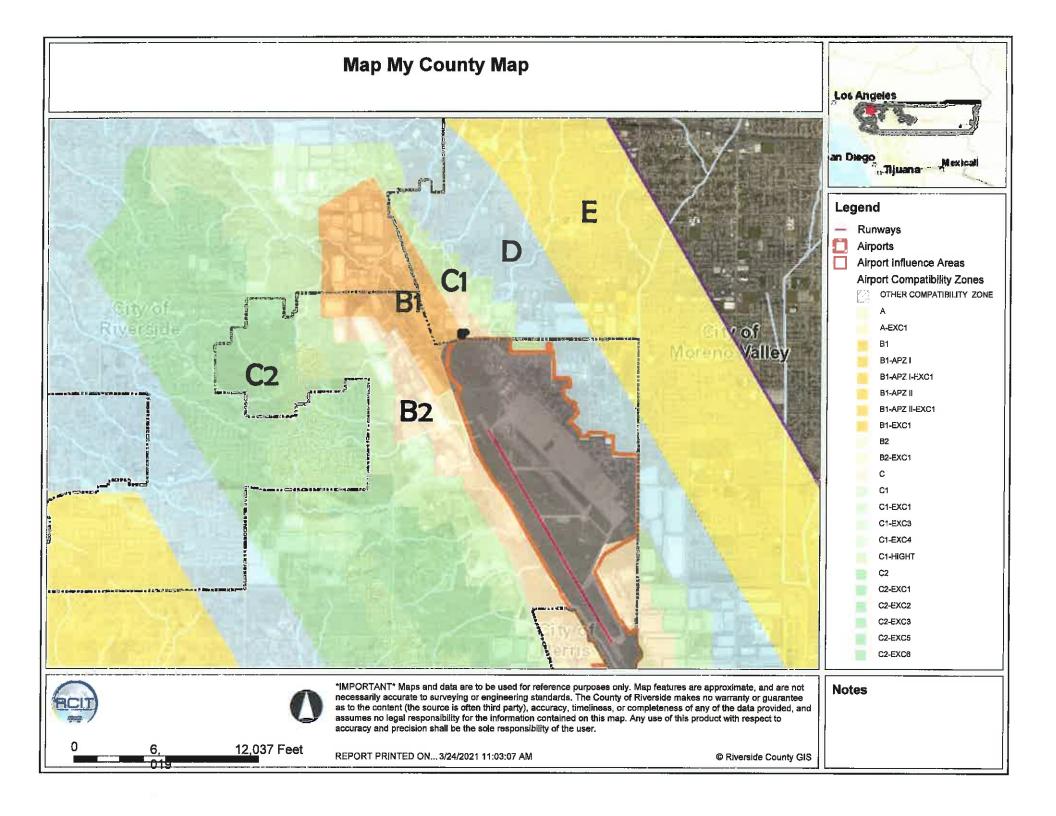
March Air Reserve Base / Inland Port Airport Land Use Compatibility Plan

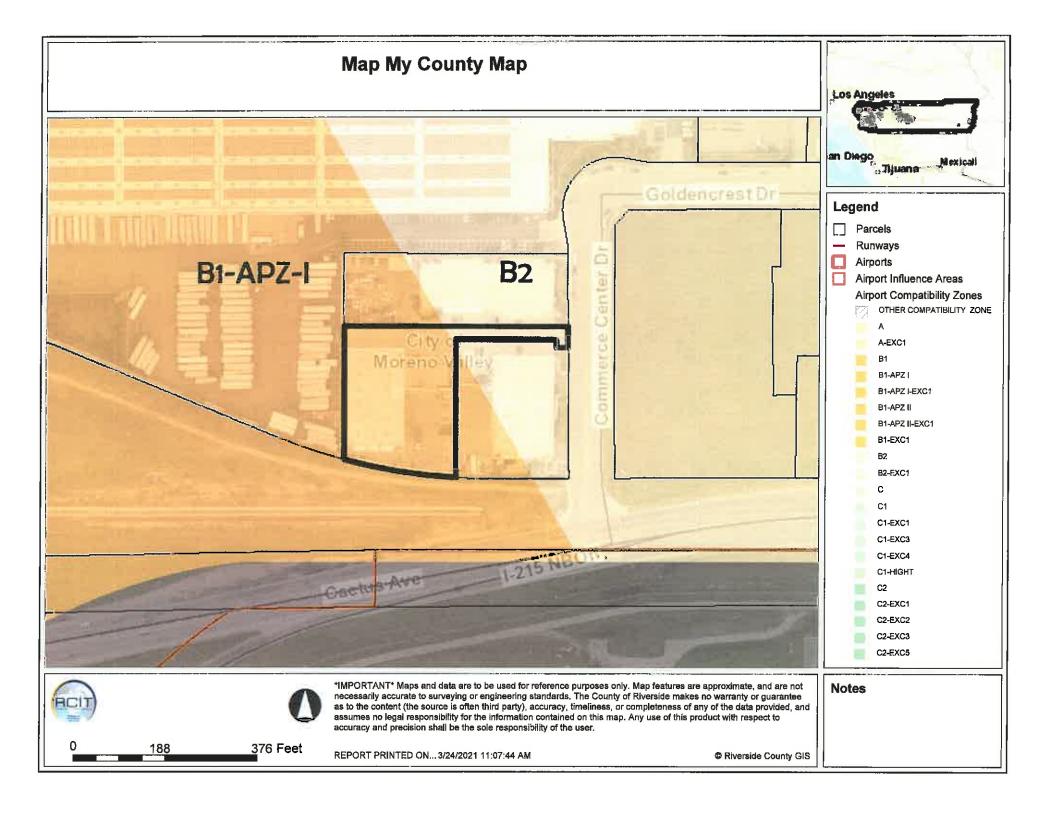
(Adopted November 13, 2014)

Map MA-1

Compatibility Map

March Air Reserve Base / Inland Port Airport









Legend

- Parcels County Centerline Names
- County Centerlines Blueline Streams
- City Areas World Street Map





IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to

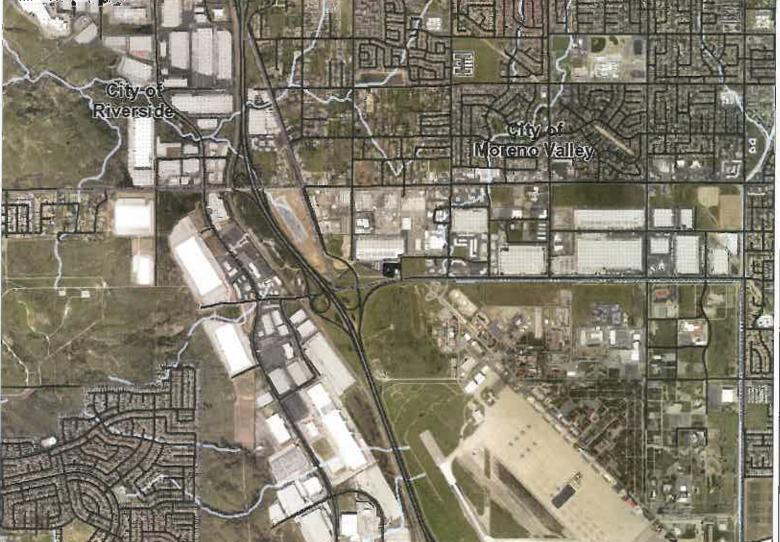
376 Feet

accuracy and precision shall be the sole responsibility of the user.

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Notes

C Riverside County GIS





Legend

County Centerlines
 Blueline Streams
 City Areas
 World Street Map





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3, 6,019 Feet

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Notes





Legend

- Parcels
- County Centerlines
 Blueline Streams
- City Areas
 World Street Map

Notes





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MOIGS





Legend

- Parcels
- County Centerlines **Blueline Streams**
- City Areas
 - World Street Map





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Notes

376 Feet

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17 - MPPT 3 - INPUT #1

19 - MPPT 4 - INPUT #1

21 - MPPT 5 - INPUT #1

23 - MPPT 6 - INPUT #1

18 - MPPT 3 - INPUT#2

20 - MPPT 4 - INPUT #2

22 - MPPT 5 - INPUT #2

24 - MPPT 6 - INPUT #2

24 24 24 24 24 24 24 24 24

21 21 21

PROPERTY LINE

SOLAR SYSTEM SIZE: 151,200 kW DC CEC AC SYSTEM SIZE: 137,738 kW AC

62KW INVERTER (A) SUNNY TRIPOWER CORE1 62-US-41 480Y/277 VOLT, 3Ф LOCATED ON ROOFTOP

250 AMP AC LOAD CENTER SIEMENS P1E18MC250AT WITH SIEMENS WP32 ENCLOSURE 480V, 3P, NEMA 3R LOCATED ON ROOFTOP

EXISTING METER #258000-022791 EXISTING 800 AMP MAIN SWITCHGEAR 480/277 VOLT, 3Φ, 4W LOCATED INSIDE UTILITY ROOM 200 AMP FUSIBLE AC DISCONNECT SQUARE-D#H364 600V, 3P, NEMA 1 LOCATED NEXT TO MAIN

62KW INVERTER (B) SUNNY TRIPOWER CORE1 62-US-41 480Y/277 VOLT, 3Ф LOCATED ON ROOFTOP

SCOPE OF WORK:

COMMON DRIVEWAY

PARKING

PARKING

Project consists of installing a roof mounted 151.200 kw DC commercial solar photovoltaic (PV) system comprised of:

(336) Sunpower 450 watt PV modules (Ungrounded)

(#SPR-A450-COM-Mt.SD), (2) STP CORE1-62-US-41 Inverter, 480/27 / VOLT, 30

(1) 250 Amp AC Load Center

(1) 200 Amp Square-D Fusible AC Disconnect Condults w/fittings and wires, Single Tilt Ballasted Helix

System, and all necessary electrical equipment to provide a renewable power source for the customer. PROJECT CONTACTS: BUILDING ZONING: I TYPE: INDUSTRIAL / BUSINESS PARK

CONTRACTOR: RENOVA permits@renovaenergy.com PRIMARY CONTACT: MATTHEW DE LA TORRE SECONDARY CONTACT: Jamie Silva

SCALE: 1" = 20"

WIND EXPOSURE: C SEISMIC DESIGN CATEGORY: D

WIND DESIGN SPEED: 110 MPH

APPLICABLE CODES-CALIFORNIA RESIDENTIAL CODE 2019 CALIFORNIA ELECTRICAL CODE 2019 NATIONAL ELECTRICAL CODE 2017 CALIFORNIA AMENDMENTS 2019 CITY OF MORENO VALLEY MUNICIPAL CODE



RIVERSIDE COUNTY: APN 297-130-062 VICINITY MAP



D Renova Energy Corp 75-161 Mediterranean Ave. Palm Desert, CA 92211 (760)568-3413 enova.ene



PV Installation Company Renova Energy Corp. California - 2012



ABRASIVES BLASTING AND (COM19799) COATING

COMMERCE CENTER DRIVE ect Name: NAUTILUS / POWDER 14451 귷

Drawn By: DAVID THORNBURY

Date Drawn:

01/04/2021

Revision No. O

Sheet No.

S1 SITE PLAN

AC LOAD CENTER/DISCONNECT

ELECTRIC SHOCK HAZARD, DO NOT TOUCH TERMINALS, TERMINALS ON TH THE LINE AND LOAD SIDES MAY BE ENERGIZED IN THE OPEN POSITION.

MAX AC OPERATING CURRENT: 159.0 A OPERATING AC VOLTAGE: 480 Volts

DC DISCONNECT INVERTER(\$)

WARNING

ELECTRIC SHOCK HAZARD, DO NOT TOUCH TERMINALS, TERMINALS ON BOTH THE LINE AND LOAD SIDES MAY BE ENERGIZED IN THE OPEN POSITION.

OPERATING CURRENT: 105.6 Amps OPERATING VOLTAGE: 616.0 Volts MAXIMUM VOLTAGE: 768.5 Volts SHORT-CIRCUIT CURRENT: 113.4 Amps

AC DISCONNECT

PHOTOVOLTAIC SYSTEM EQUIPPED WITH RAPID SHUTDOWN

TURN RAPID SHUTDOWN SWITCH TO THE "OFF" POSITION TO SHUT DOWN PV SYSTEM AND REDUCE SHOCK HAZARD IN THE ARRAY



MAIN PANEL

THIS PANEL IS FED BY TWO SOURCES: UTILITY AND SOLAR PHOTOVOLTAIC

AC DISCONNECT

RAPID SHUTDOWN SWITCH FOR SOLAR PV SYSTEM

LOAD CENTER

PHOTOVOLTAIC DISCONNECT BREAKER LOCATED INSIDE

MAIN PANEL

PHOTOVOLTAIC SYSTEM EQUIPPED WITH RAPID SHUTDOWN

JUNCTION BOX

WARNING

ELECTRIC SHOCK HAZARD. THE DC CONDUCTORS OF THE PHOTOVOLTAIC SYSTEM ARE UNGROUNDED AND MAY BE ENERGIZED.

INVERTER

WARNING

ELECTRIC SHOCK HAZARD IF A GROUND FAULT IS INDICATED, THE NORMALLY GROUNDED CONDUCTORS MAY BE UNGROUNDED AND ENERGIZED,

SOLAR BREAKER

WARNING

INVERTER OUTPUT CONNECTION DO NOT RELOCATE THIS OVER CURRENT

SAFEGUARDS

- 1. SOLAR BREAKER SHALL BE LISTED FOR USE IN EXISTING MAIN PANEL. ACCEPTED BREAKER REPLACEMENTS ARE EATON, SQUARE-D & GE.
- 2. ALL JUNCTION BOXES SHALL BE NEMA OUTDOOR/RAINTIGHT RATED.
- 3. WILEY CABLE CLIPS SHALL BE USED.
- 4. ALL EMT SHALL USE OUTDOOR RAINTIGHT COMPRESSION FITTINGS.
- 5. ALL EQUIPMENT SHALL BE LABELED.
- 6. ADD COMBINER / JUNCTION BOXES AS NEEDED.
- 7. AN INTERNAL BLOCKING DIODE IS PROVIDED IN EACH MODULE. 8. GROUND FAULT PROTECTION DEVICE IS INTERNALLY BUILT
- INTO SUNPOWER INVERTERS. 9. MODULE GROUNDING SHALL BE MADE WITH ILSCO GBL-4DB
- LUGS AND #8 SOLID BARE COPPER USING WEEB CONNECTIONS
- 10. ALL NEUTRAL TO GROUND CONNECTIONS SHALL OCCUR IN THE MAIN PANEL.
- 11. EMT SHALL BE 1" MINIMUM.
- JUNCTION BOX/ COMBINER BOX ENTRY SHALL USE COMPRESSION FITTINGS.

MARKING

- I. THE MATERIAL USED FOR MARKING SHALL BE REFLECTIVE, WEATHER RESISTANT AND SUITABLE FOR THE ENVIRONMENT, MARKING AS REQUIRED IN SECTIONS R 331.2.2 THROUGH R331.2.4 SHALL HAVE LETTERS CAPITALIZED WITH A MINIMUM HEIGHT OF 3/8" WHITE ON RED BACKGROUND, (CRC R331,2.1)
- 2. THE MARKING CONTENT SHALL CONTAIN THE WORDS "WARNING; PHOTOVOLTAIC POWER SOURCE" (CRC R331.2.2) NOTES
- WIRE AND SIZES ARE AS INDICATED OR LARGER, TEMPERATURE RATING WILL BE 90°C.
- 2. ALL EQUIPMENT IS BONDED BY MECHANICAL MEANS OR BY A GROUNDING CONDUCTOR.
- 3. THE GROUNDING ELECTRODE CONDUCTOR SHALL BE #8 AWG COPPER GROUND WIRE AND BONDED TO THE EXISTING GROUNDING ELECTRODE SYSTEM.
- 4. CHANGES TO DESIGN NEED PRIOR ENGINEER APPROVAL.

PANEL: LOAD CENTER LOCATION: ROOFTOP MOUNTING: SURFACE

CIRCUIT CODE: N: NON-CONTINUOUS

L: LONG-CONTINUOUS

R: DEMANDABLE RECEPTACLES

PV-WIRE AND AC CABLE.

14. ALL CONDUIT SHALL BE 1" EMT OR LARGER.

16. ALL CONDUIT SHALL BE SUPPORTED BY RUBBER

18. ALL CONDUIT SHALL HAVE PLASTIC STRAIN RELIEF

17. STEEL CITY 6"X6"X4" (OR LARGER) OUTDOOR

JUNCTION BOXES SHALL BE USED AS NEEDED.

15. ALL CONDUIT SHALL BE BONDED.

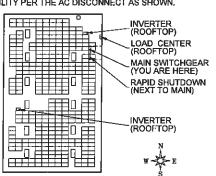
PYRAMID SUPPORTS AS REQUIRED.

CONNECTORS TO PROTECT WIRES.

NOTE: RED BACKGROUND WITH WHITE LETTERING 3/8" LETTER HEIGHT

MAIN PANEL: MECHANICALLY ATTACHED

WARNING 🕭 THIS FACILITY IS EQUIPPED WITH A PHOTOVOLTAIC GENERATOR LOCATED ON THE ROOF AS SHOWN.
THE SYSTEM MAY BE DISCONNECTED FROM THE FACILITY PER THE AC DISCONNECT AS SHOWN.



PANEL VOLTAGE: 480/277 V 3Φ.4W BUS: 250A

VA LOAD			LOAD								CODE	Т	YPE	Ξ.	C.	В.	В	JS	=	C.	В.		ГҮР	E	CODE	I.OAD	VAL	OAD
Α	В	С	DESCRIPTION	ၓ	N	R	Ĺ	TR	Р	Α	В	C	Р	TR	L	R	N	ខ	DESCRIPTION	В	A							
20833			INVERTER A	L			100	100	3	х								L										
	20833		INVERTER A	L			-	-	-		х							L										
		20833	INVERTER A	L			-	-	-			х						L										
20833			INVERTER B	ب			100	1CO	3	×								L										
	20833		INVERTER B	L			-	-	-		х							7										
		20833	INVERTER B	L			-	- ,	-			х						L										
					L																							
41666	41666	41666	PHASE TOTAL																									

PHASE TOTALS A+B+C | CONNECTED VA (CODE L): | 124998 CONNECTED VA (CODE R): CONNECTED VA (CODE N)

TOTAL CONNECTED KVA: 125.0 TOTAL PANEL AMPS: 159,0

PROJECT SIGNAGE

NOTES & PANEL SCHEDULE

13. ALL WIRING BETWEEN AC MODULES SHALL BE

NC. 18800 EXP. 6/30/22 energy Renova Energy Corp 75-181 Mediterranean Ave. Palm Desert, CA 92211 (760)568-3413 enova.

ACCREDITED

PV Installation Company Renova Energy Corp. California - 2012



ABRASIVES BLASTING AND (COM19799) COATING LLC

92553

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VALLEY

MORENO

CENTER DRIVE COMMERCE NAUTILUS POWDER 14451

Drawn By: 277 DAVID THORNBURY

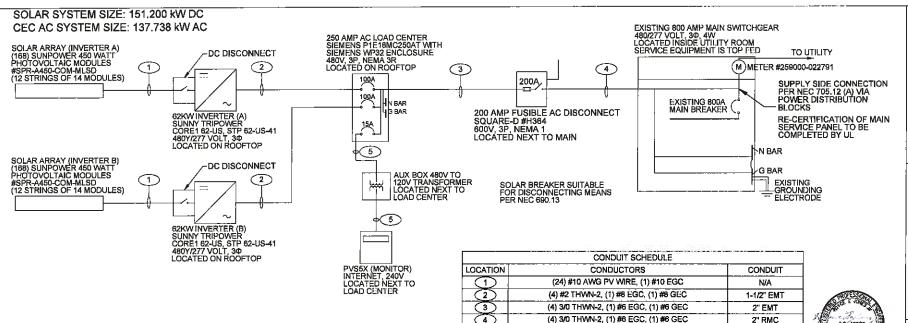
Date Drawn: 01/04/2021

Revision No

0

Sheet No.

E1 PROJECT SIGNAGE



NO. 18800 EXP. 8/30/22

3/4" EMT

ALL WIRING BETWEEN MODULES SHALL BE TYPE PV-WIRE.

ALL CONDUIT SHALL BE BONDED.
STEEL CITY 12"X12"X4" (OR LARGER) OUTDOOR JUNCTION BOXES SHALL BE USED AS NEEDED.
ALL CONDUIT SHALL HAVE PLASTIC STRAIN RELIEF CONNECTORS TO PROTECT WIRES.

DC CIRCUIT CALCULATIONS

MAX VOLTAGE DC = (MODULE Voc + ((25°C - Temp Record Low*C [via www.solarabcs.org]) x T coefV)) X # modules in series

MAX VOLTAGE DC= (51.9 +((25°C - 3°C) x .1360)) X 14 modules in series = 768.5 VOLTS

11.0 AMPS EACH CIRCUIT X 1.56[PER NEC 690.8] = 17.2 AMPS

DC CONDUIT SHALL BE MOUNTED A MINIMUM OF 1" ABOVE ROOFTOP SURFACE.

ENGINEERED FOR 90°C TEMP, RATING OF CONDUCTOR & 44°C AMBIENT TEMP, PER SQLARABCS.ORG

MODULES TO INVERTER A

12 STRINGS X 14 MODULES = 168 MODULES

#10 WRE = 40 AMPS X .82[PER NEC TABLE 310.15(B)(16) & PER NEC TABLE 690.31(A)] = 32.8 AMPS ≥ 17.2 AMPS ✓

INVERTER A: 12 STRINGS= (24) #10 AWG PV WIRE, (1) #10 THWN-2 GROUND

PV WIRE RUNS UNDER SOLAR MODULES, SUPPORTED BY HELIX ROOF CABLE MANAGEMENT SYSTEM

MODULES TO INVERTER B

12 STRINGS X 14 MODULES = 168 MODULES

#10 WIRE = 40 AMPS X .82[PER NEC TABLE 310.15(B)(16) & PER NEC TABLE 690.31(A)] = 32.8 AMPS 32.8 AMPS X .80[CONDUIT FILL PER NEC TABLE 310.15(B)(3)(a)] = 26.24 AMPS > 17.2 AMPS ✓

INVERTER B: 12 STRINGS= (24) #10 AWG PV WIRE, (1) #10 THWN-2 GROUND

2 STRINGS PER CONDUIT (TRANSITION FROM ARRAY TO ARRAY - SEE SHEET P1) =

(4) #10 AWG PV WIRE, (1) #10 THWN-2 BARE GROUND EGC

1" EMT @ 40% FILL PER NEC TABLE C.1 = (16) #10 THWN-2 V

PV WIRE RUNS UNDER SOLAR MODULES, SUPPORTED BY HELIX ROOF CABLE MANAGEMENT SYSTEM

INVERTER A TO 250A AC LOAD CENTER

INVERTER A MAX OUTPUT = 79.5A X 1.25(PER NEC 215.2(A)11 = 99.4 AMPS (100 AMP SOLAR BREAKER)

#2 WRE = 130 AMPS X .82[PER NEC TABLE 310.15(B)(16)] = 106.5 AMP ≥ 100 AMP ✓

(3) #12 THWN-2, (1) #12 EGC

(4) #2 THWN-2 AWG WIRE, (1) #8 THWN-2 GROUND, (1) #8 THWN-2 GROUND

1-1/2" EMT @ 40% FILL PER NEC TABLE C.1 = (7) #2 THWN-2 WIRES ✓

INVERTER B TO 250A AC LOAD CENTER

INVERTER B MAX OUTPUT = 79.5A X 1.25[PER NEC 215.2(A)1] = 99.4 AMPS (100 AMP SOLAR BREAKER)

#2 WIRE = 130 AMPS X .82[PER NEC TABLE 310.15(B)(16)] = 106.5 AMP ≥ 100 AMP ✓

(4) #2 THWN-2 AWG WIRE, (1) #8 THWN-2 GROUND, (1) #8 THWN-2 GROUND

1-1/2" EMT @ 40% FILL PER NEC TABLE C.1 = (7) #2 THWN-2 WIRES ✓

250A AC LOAD CENTER TO MAIN PANEL

MAX OUTPUT = (79.5A + 79.5A) X 1.25[PER NEC 215.2(A)1] = 198.8 AMPS (200 AMP FUSES)

AC CONDUIT SHALL BE MOUNTED ON EXTERIOR WALL

3/0 WIRE = 225 AMPS ≥ 200 AMP √

(4) 4/0 THWN-2 AWG WIRE, (1) #6 THWN-2 GROUND, (1) #6 THWN-2 GROUND

2" EMT @ 40% FILL PER NEC TABLE C.1 = (5) 3/0 THWN-2 WIRES V

2" RMC @ 40% FILL PER NEC TABLE C.9 = (5) 3/0 THWN-2 WIRES V

MAIN PANEL BACKFEED CALCULATION

EXISTING 800 AMP MAIN SWITCHGEAR

SUPPLY SIDE CONNECTION SHALL BE PERFORMED PER NEC 705.12(A)

SINGLE LINE DIAGRAM

NAUTILUS ABRASIVES BLASTING AND (COM19799) COATING LLC **POWDER**

CENTER DRIVE

COMMERCE

92553

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VALLEY,

energi

enova.

ACCREDITED

PV Installation

Company

Renova Energy Corp. California - 2012

Renova Energy Corp 75-181 Mediterranean Ave. Palm Desert, CA 92211 (760)568-3413 Contractor's Licensa #910836

MORENO 14451 Drawn By: 277

DAVID THORNBURY

Date Drawn 01/04/2021

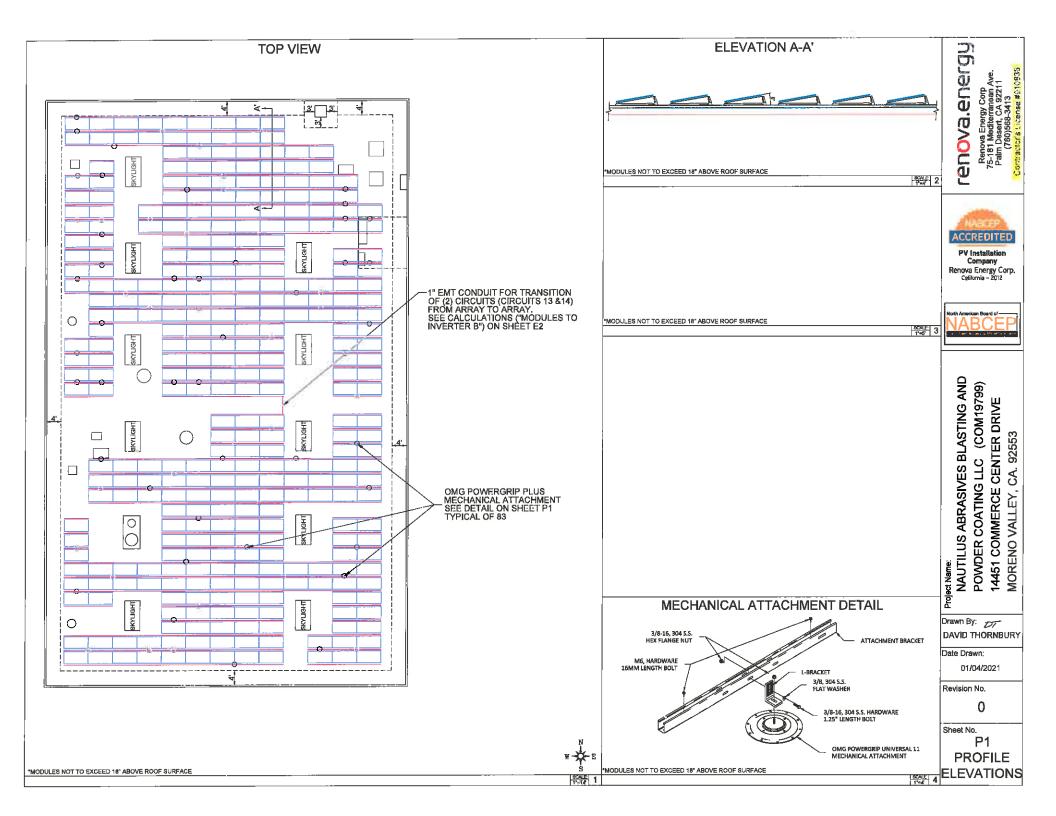
Revision No.

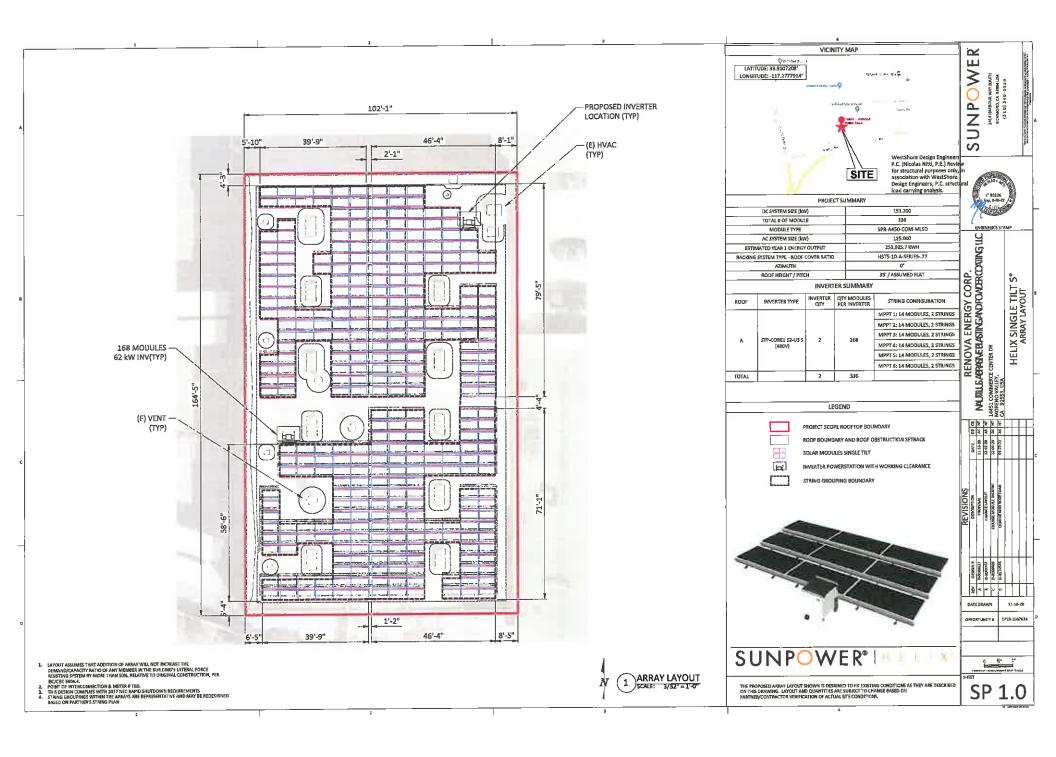
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Sheet No.

E2 SINGLE LINE

CALCULATIONS







Engineering Report for Solar Glare Analysis

Nautilus Abrasives Blasting and Powder Coating, LLC., 14451 Commerce Center Drive, Moreno Valley, CA 92553

March 11, 2021

Renova Energy Corp. has a contract with Nautilus Abrasives Blasting and Powder Coating, LLC. located at 14451 Commerce Center Drive in Moreno Valley, CA 92553. The contract is for a new 151.2 kW ballasted solar photovoltaic system that shall be installed on the rooftop of the existing structure. The solar array is comprised of (336) SunPower SPR-A450-COM-MLSD 450 watt modules and a single-tilt ballasted PanelClaw mounting system. The specifications for the proposed solar photovoltaic system, photovoltaic modules and racking system are included in this package.

The project was submitted to the building department of the City of Moreno Valley and it was determined it was within range of the March Air Reserve Base (March AFB) and needed further approval from the Airport Land Use Commission. The Authority Having Jurisdiction requested a Solar Glare Hazard Analysis be completed in order to prove no excessive glint or glare will be created by the project to interfere with pilots or the control tower operating this facility.

Utilizing ForgeSolar Glare Study tool, a final glare model and analysis was completed for the site and solar photovoltaic system. The results of the glare study found to PASS analyses compliant with the Airport Land Use Commission, FAA and USAF regulations in all three components provided including: Analysis Parameters, Flight Paths and ATCTs. The inputs, model parameters and results from the analysis are document and attached.

Summarized inputs and findings can be found below. Complete documents are included in the packet.



Corporate Headquarters 75-181 Mediterranean Ave. | Palm Desert, CA 92211

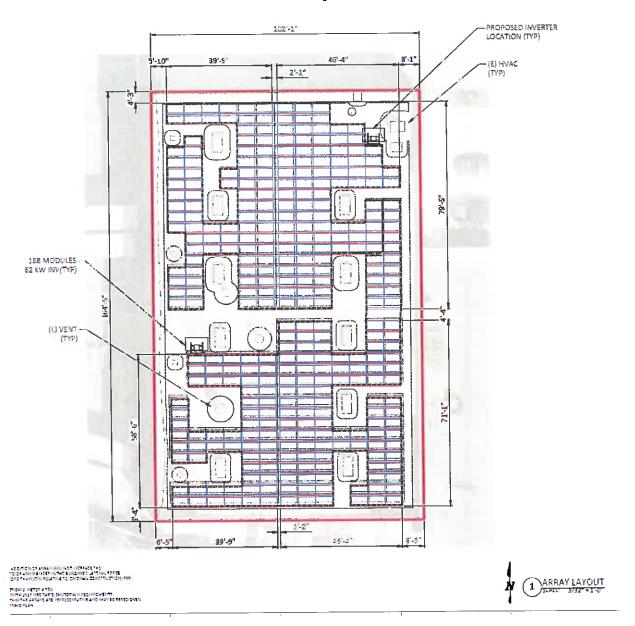
Vicinity Map





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Solar PV System





Corporate Headquarters

75-181 Mediterranean Ave. | Palm Desert, CA 92211

Summarized Solar Glare Inputs

SITE CONFIGURATION

Analysis Parameters

DNI: peaks at 1,000.0 W/m²2 Time interval: 1 min Cloular transmission coefficient: 0.3 Pupil clamatar: 0,002 m Eys focal langth: 0,017 m Sun subtended angle: 9.3 mrad Sits Config ID: 50639,9032



PV Array(s)

Name: PV array 5

Axis tracking: Fixed (no rotation)

71h: 5.0°

Orientation: 180.0° Rated power: 151.2 kW

Panel material: Smooth glass without AR couling

Reflectivity: Vary with son Stops error: correlate with material



Vertex	Latitude (*)	Longituda (*)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
1	33,910885	-117.275868	1554.39	20.00	1574.38
2	33.910389	-117.275561	1555.98	20.00	1575,98
3	33.910450	-117.278880	1557.88	20.00	1577.88
14	33.910451	-117,275973	1988.08	20.00	1676108

Corporate Headquarters

75-181 Mediterranean Ave. Palm Desert, CA 92211

Summarized Solar Glare Findings

FORGESOLAR GLARE ANALYSIS

Project: Nautilus

Proposed PV Rooftop (396) SunPower SPR-A450-COM-MLSD Helix.

Site configuration: South Single Tilt at 5 Degrees - 3-5-21

Analysis conducted by Matthew De La Torre (mdelatorre@renovaenergy.com) at 22:09 on 05 Mar, 2021.

U.S. FAA 2013 Policy Adherence

The following table summarizes the policy adherence of the glare analysis based on the 2013 U.S. Federal Aviation Administration Interfit Policy 78 FR 63276. This policy requires the following criteria be mat for solar energy systems on airport property:

- Mo "yellow" glare (potential for after-image) for any flight path from threshold to 2 miles
- the No glare of any kind for Air Traffic Control Tower(s) ("ATCT") at cab height.
- Default analysis and observer characteristics (see list below).

ForgaSolar does not represent or speak officially for the FAA and cannot approve or deny projects. Pleaults are informational only.

COMPONENT	STATUS	DESCRIPTION
Analysis parameters	PASS	Analysis time interval and eye characteristics used are acceptable
2-mile flight path(s)	PASS	Flight path receptor(s) do not receive yellow glare
ATCT(s)	PASS	Receptor(a) marked as ATCT do not receive glare



FORGESOLAR GLARE ANALYSIS

Project: Nautilus

Proposed PV Rooftop (336) SunPower SPR-A450-COM-MLSD Helix.

Site configuration: South Single Tilt at 5 Degrees - 3-5-21

Analysis conducted by Matthew De La Torre (mdelatorre@renovaenergy.com) at 22:09 on 05 Mar, 2021.

U.S. FAA 2013 Policy Adherence

The following table summarizes the policy adherence of the glare analysis based on the 2013 U.S. Federal Aviation Administration Interim Policy 78 FR 63276. This policy requires the following criteria be met for solar energy systems on airport property:

- No "yellow" glare (potential for after-image) for any flight path from threshold to 2 miles
- No glare of any kind for Air Traffic Control Tower(s) ("ATCT") at cab height.
- Default analysis and observer characteristics (see list below)

ForgeSolar does not represent or speak officially for the FAA and cannot approve or deny projects. Results are informational only.

COMPONENT	STATUS	DESCRIPTION
Analysis parameters	PASS	Analysis time interval and eye characteristics used are acceptable
2-mile flight path(s)	PASS	Flight path receptor(s) do not receive yellow glare
ATCT(s)	PASS	Receptor(s) marked as ATCT do not receive glare

Default glare analysis parameters and observer eye characteristics (for reference only):

Analysis time interval: 1 minute

· Ocular transmission coefficient: 0.5

Pupil diameter: 0.002 meters

Eye focal length: 0.017 meters

· Sun subtended angle: 9.3 milliradians

FAA Policy 78 FR 63276 can be read at https://www.federalregister.gov/d/2013-24729

SITE CONFIGURATION

Analysis Parameters

DNI: peaks at 1,000.0 W/m^2

Time interval: 1 min Ocular transmission coefficient: 0.5

Pupil diameter: 0.002 m Eye focal length: 0.017 m Sun subtended angle: 9.3

mrad

Site Config ID: 50639.9052



PV Array(s)

Name: PV array 1

Axis tracking: Fixed (no rotation)

Tilt: 5.0°

Orientation: 180.0° Rated power: 151.2 kW

Panel material: Smooth glass without AR coating

Reflectivity: Vary with sun

Slope error: correlate with material



Vertex	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
1	33.910885	-117.275868	1554.38	20.00	1574.38
2	33.910888	-117.275551	1555.98	20.00	1575.98
3	33.910450	-117.275550	1557.88	20.00	1577.88
4	33.910451	-117.275873	1556.08	20.00	1576.08

Flight Path Receptor(s)

Name: C/KC, Rwy 14 Base Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.922394	-117.325047	1500.07	1500.07	3000.15
Two-mile	33.931244	-117.309014	1500.07	1500.07	3000.15

Name: C/KC Rwy 14 Crosswind

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.821961	-117.228367	1500.07	1500.07	3000.15
Two-mile	33.813147	-117.244350	1500.07	1500.07	3000.15

Name: C/KC, Rwy 14 Downwind

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.819225	-117.262269	1500.07	1500.07	3000.15
Two-mile	33.908131	-117.325528	1500.07	1500.07	3000.15

Name: C/KC, Rwy 14 Final Description: None Threshold helght: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.925156	-117.291061	1500.07	1500.07	3000.15
Two-mile	33.896431	-117.270636	1500.07	0.00	1500.07

Name: C/KC, Rwy 14 Upwind

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.864994	-117.248281	1500.07	0.00	1500.07
Two-mile	33.836269	-117.227869	1500.07	1500.07	3000.15

Name: C/KC, Rwy 32 Base Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.813147	-117.244350	1500.07	1500.07	3000.15
Two-mile	33.821961	-117.228367	1500.07	1500.07	3000.15

Name: C/KC, Rwy 32 Crosswind

Description: None Threshold height: 0 ft Direction: 314.8° Gilde slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.931244	-117.309014	1500.07	1500.07	3000.15
Two-mile	33.922394	-117.325047	1500.07	1500.07	3000.15

Name: C/KC, Rwy 32 Downwind

Description: None Threshold height: 0 ft Direction: 314.8° Gilde slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.908131	-117.325528	1500.07	1500.07	3000.15
Two-mile	33.819225	-117.262269	1500.07	1500.07	3000.15

Name: C/KC, Rwy 32 Final Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.836269	-117.227869	1500.07	1500.07	3000.15
Two-mile	33.864994	-117.248281	1500.07	0.00	1500.07

Name: C/KC, Rwy 32 Upwind

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.896431	-117.270636	1500.07	0.00	1500.07
Two-mile	33.925156	-117.291061	1500.07	1500.07	3000.15

Name: Ga, Rwy 12 Base Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azlmuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.910322	-117.264967	1500.07	1300.06	2800.14
Two-mile	33.905592	-117.270622	1500.07	1300.06	2800.14

Name: GA, Rwy 12 Crosswind

Description: None Threshold height: 0 ft Direction: 314.8° Gilde slope: 5.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.876081	-117.235119	1500.07	1300.06	2800.14
Two-mile	33.880614	-117.229467	1500.07	1300.06	2800.14

Name: GA, Rwy 12 Downwind

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Helght above ground (ft)	Total elevation (ft)
Threshold	33.887897	-117.229483	1500.07	1300.06	2800.14
Two-mile	33.910333	-117.256469	1500.07	1300.06	2800.14

Name: GA, Rwy 12 Final Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.898508	-117.270608	1500.07	1300.06	2800.14
Two-mile	33.890258	-117.260681	1500.07	0.00	1500.07

Name: GA, Rwy 14 Base Description: None Threshold height: 0 ft Direction: 314.8° Gilde slope: 5.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.904833	-117.292903	1500.07	1500.07	3000.15
Two-mile	33.908242	-117.286017	1500.07	1500.07	3000.15

Name: GA, Rwy 14 Crosswind

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.848078	-117.243236	1500.07	1500.07	3000.15
Two-mile	33.844669	-117.250119	1500.07	1500.07	3000.15

Name: GA, Rwy 14 Downwind

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view; 30.0° Azimuthal view; 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.846422	-117.258344	1500.07	1500.07	3000.15
Two-mile	33.897972	-117.295011	1500.07	1500.07	3000.15

Name: GA, Rwy 14 Final Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°



Point	Latitude (")	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.906486	-117.277783	1500.07	1500.07	3000.15
Two-mile	33.896431	-117.270636	1500.07	0.00	1500.07

Name: GA, Rwy 14 Upwind Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.864994	-117.248281	1500.07	0.00	1500.07
Two-mile	33.854942	-117.241136	1500.07	1500.07	3000.15

Name: GA, Rwy 30 Base Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azlmuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.880814	-117.229467	1500.07	1300.06	2800.14
Two-mile	33.876081	-117.235119	1500.07	1300.06	2800.14

Name: Ga, Rwy 30 Crosswind

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

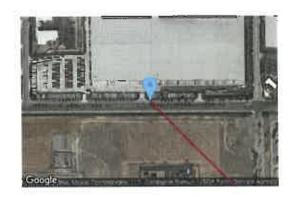


Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.905592	-117.270622	1500.07	1300.06	2800.14
Two-mile	33.910322	-117.264967	1500.07	1300.06	2800.14

Name: Ga, Rwy 30 Downwind

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.910333	-117.256469	1500.07	1300.06	2800.14
Two-mile	33.887897	-117.229483	1500.07	1300.06	2800.14

Name: GA, Rwy 30 Final Description: None Threshold height: 0 ft Direction: 314.8° Gilde slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.876069	-117.243611	1500.07	1300.06	2800.14
Two-mile	33.884319	-117.253536	1500.07	0.00	1500.07

Name: Ga, Rwy 30 Upwind Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.890258	-117.260681	1500.07	0.00	1500.07
Two-mile	33.898508	-117.270608	1500.07	1300.06	2800.14

Name: GA, Rwy 32 Base Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°

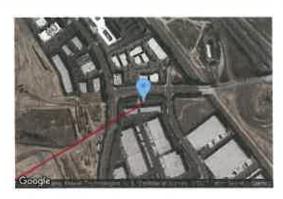


Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.844669	-117.250119	1500.07	1500.07	3000.15
Two-mile	33.848078	-117.243236	1500.07	1500.07	3000.15

Name: Ga, Rwy 32 Crosswind

Description: None Threshold helght: 0 ft Direction: 314.8° Glide slope: 5.0°

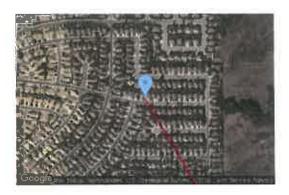
Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.908242	-117.286017	1500.07	1500.07	3000.15
Two-mile	33.904833	-117.292903	1500.07	1500.07	3000.15

Name: Ga, Rwy 32 Downwind

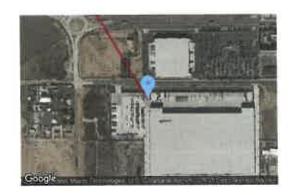
Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.897972	-117.295011	1500.07	1500.07	3000.15
Two-mile	33.846422	-117.258344	1500.07	1500.07	3000.15

Name: GA, Rwy 32 Final Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

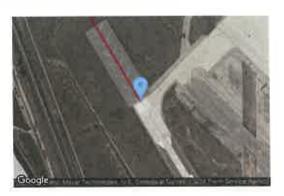
Pliot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.854942	-117.241136	1500.07	1500.07	3000.15
Two-mile	33.864994	-117.2 48281	1500.07	0.00	1500.07

Name: GA, Rwy 32 Upwind Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.896431	-117.270636	1500.07	0.00	1500.07
Two-mile	33.906486	-117.277783	1500.07	1500.07	3000.15

Name: Ol-lead, Rwy 14 Downwind

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.863564	-117.293808	1500.07	2000.10	3500.17
Two-mile	33.908131	-117.325528	1500.07	2000.10	3500.17

Name: OHead, Rwy 14 Final

Description: None Threshold height: 0 ft Direction: 314.8° Gilde slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ff)	Height above ground (ft)	Total elevation (ft)
Threshold	33.925156	-117.291061	1500.07	2000.10	3500.17
Two-mile	33.896431	-117.270636	1500.07	0.00	1500.07

Name: OHead, Rwy 14 Initial

Description: None Threshold helght: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.968036	-117.322128	1500.07	2000.10	3500.17
Two-mile	33.880706	-117.259453	1500.07	2000.10	3500.17

Name: OHead, Rwy 32 Downwind

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.863564	-117.293808	1500.07	2000.10	3500.17
Two-mile	33.819225	-117.262269	1500.07	2000.10	3500.17

Name: OHead, Rwy 32 Final

Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°

Pilot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.836269	-117.227869	1500.07	2000.10	3500.17
Two-mile	33.864994	-117.248281	1500.07	0.00	1500.07

Name: OHead, Rwy 32 Initial

Description: None Threshold height: 0 ft Direction: 314.8° Gilde slope: 5.0°

Pliot view restricted? Yes Vertical view: 30.0° Azimuthal view: 50.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.793375	-117.196878	1500.07	2000.10	3500.17
Two-mile	33.880706	-117.259453	1500.07	2000.10	3500.17

Name: Rwy 12-Upwind Description: None Threshold height: 0 ft Direction: 314.8° Glide slope: 5.0°



Point	Latitude (°)	Longitude (°)	Ground elevation (ft)	Height above ground (ft)	Total elevation (ft)
Threshold	33.884319	-117.253536	1500.07	0.00	1500.07
Two-mile	33.876069	-117.243611	1500.07	1300.06	2800.14

Discrete Observation Receptors

Name	ID	Latitude (°)	Longitude (°)	Elevation (ft)	Height (ft)
1-ATCT	1	33.891564	-117.251171	1508.77	18.00

Map image of 1-ATCT



GLARE ANALYSIS RESULTS

Summary of Glare

PV Array Name	Tilt	Orient	"Green" Glare	"Yellow" Glare	Energy
	(°)	(°)	min	min	kWh
PV array 1	5.0	180.0	7,632	0	325,700.0

Total annual glare received by each receptor

Receptor	Annual Green Glare (min)	Annual Yellow Glare (min)
C/KC, Rwy 14 Base	O	0
C/KC Rwy 14 Crosswind	O	0
C/KC, Rwy 14 Downwind	0	0
C/KC, Rwy 14 Final	0	O
C/KC, Rwy 14 Upwind	0	O
C/KC, Rwy 32 Base	0	0
C/KC, Rwy 32 Crosswind	36	0
C/KC, Rwy 32 Downwind	0	0
C/KC, Rwy 32 Final	0	0
C/KC, Rwy 32 Upwind	2456	0
Ga, Rwy 12 Base	0	0

Receptor	Annual Green Glare (min)	Annual Yellow Glare (min)
GA, Rwy 12 Crosswind	0	0
GA, Rwy 12 Downwind	o	0
GA, Rwy 12 Final	o	0
GA, Rwy 14 Base	0	0
GA, Rwy 14 Crosswind	0	0
GA, Rwy 14 Downwind	0	0
GA, Rwy 14 Final	0	0
GA, Rwy 14 Upwind	0	0
GA, Rwy 30 Base	0	0
Ga, Rwy 30 Crosswind	310	0
Ga, Rwy 30 Downwind	1888	0
GA, Rwy 30 Final	0	0
Ga, Rwy 30 Upwind	0	0
GA, Rwy 32 Base	0	0
Ga, Rwy 32 Crosswind	2942	o
Ga, Rwy 32 Downwind	О	0
GA, Rwy 32 Final	0	o
GA, Rwy 32 Upwind	0	0
OHead, Rwy 14 Downwind	0	0
OHead, Rwy 14 Final	o	0
OHead, Rwy 14 Initial	o	0
OHead, Rwy 32 Downwind	0	0
OHead, Rwy 32 Final	0	0
OHead, Rwy 32 Initial	0	O
Rwy 12-Upwind	o	0
1-ATCT	0	0

Results for: PV array 1

Receptor	Green Glare (min)	Yellow Glare (min)
C/KC, Rwy 14 Base	0	0
C/KC Rwy 14 Crosswind	o	0
C/KC, Rwy 14 Downwind	o	0
C/KC, Rwy 14 Final	o	0
C/KC, Rwy 14 Upwind	o	0
C/KC, Rwy 32 Base	0	0
C/KC, Rwy 32 Crosswind	36	0
C/KC, Rwy 32 Downwind	O	0
C/KC, Rwy 32 Final	O	0
C/KC. Bwy 32 Upwind	2456	O

Receptor	Green Glare (min)	Yellow Glare (min)
Ga, Rwy 12 Base	0	0
GA, Rwy 12 Crosswind	0	0
GA, Rwy 12 Downwind	0	0
GA, Rwy 12 Final	0	0
GA, Rwy 14 Base	0	0
GA, Rwy 14 Crosswind	0	0
GA, Rwy 14 Downwind	0	0
GA, Rwy 14 Final	0	0
GA, Rwy 14 Upwind	0	0
GA, Rwy 30 Base	0	0
Ga, Rwy 30 Crosswind	310	0
Ga, Rwy 30 Downwind	1888	0
GA, Rwy 30 Final	0	0
Ga, Rwy 30 Upwind	0	0
GA, Rwy 32 Base	0	0
Ga, Rwy 32 Crosswind	2942	0
Ga, Rwy 32 Downwind	0	0
GA, Rwy 32 Final	0	0
GA, Rwy 32 Upwind	0	0
OHead, Rwy 14 Downwind	0	0
OHead, Rwy 14 Final	0	0
OHead, Rwy 14 Initial	0	0
OHead, Rwy 32 Downwind	0	0
OHead, Rwy 32 Final	0	0
OHead, Rwy 32 Initial	0	0
Rwy 12-Upwind	0	0
1-ATCT	0	0

Flight Path: C/KC, Rwy 14 Base

0 minutes of yellow glare 0 minutes of green glare

Flight Path: C/KC Rwy 14 Crosswind

0 minutes of yellow glare 0 minutes of green glare

Flight Path: C/KC, Rwy 14 Downwind

0 minutes of yellow glare 0 minutes of green glare

Flight Path: C/KC, Rwy 14 Final

0 minutes of yellow glare 0 minutes of green glare

Flight Path: C/KC, Rwy 14 Upwind

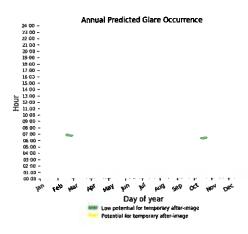
0 minutes of yellow glare 0 minutes of green glare

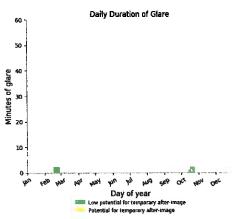
Flight Path: C/KC, Rwy 32 Base

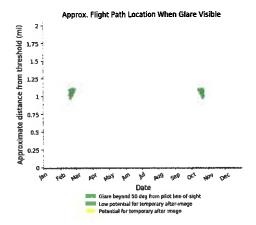
0 minutes of yellow glare 0 minutes of green glare

Flight Path: C/KC, Rwy 32 Crosswind

0 minutes of yellow glare 36 minutes of green glare







Flight Path: C/KC, Rwy 32 Downwind

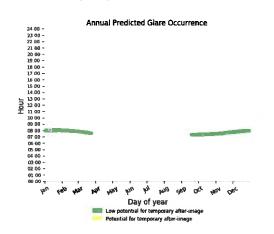
0 minutes of yellow glare 0 minutes of green glare

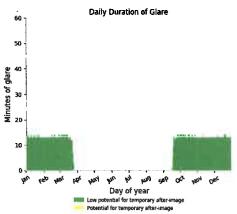
Flight Path: C/KC, Rwy 32 Final

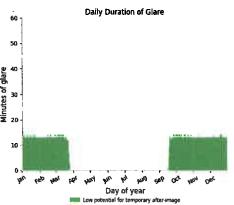
0 minutes of yellow glare 0 minutes of green glare

Flight Path: C/KC, Rwy 32 Upwind

0 minutes of yellow glare 2456 minutes of green glare

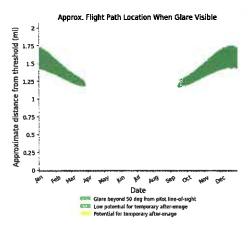






Flight Path: Ga, Rwy 12 Base

0 minutes of yellow glare 0 minutes of green glare



Flight Path: GA, Rwy 12 Crosswind

0 minutes of yellow glare 0 minutes of green glare

Flight Path: GA, Rwy 12 Downwind

0 minutes of yellow glare 0 minutes of green glare

Flight Path: GA, Rwy 12 Final

0 minutes of yellow glare 0 minutes of green glare

Flight Path: GA, Rwy 14 Base

0 minutes of yellow glare 0 minutes of green glare

Flight Path: GA, Rwy 14 Crosswind

0 minutes of yellow glare 0 minutes of green glare

Flight Path: GA, Rwy 14 Downwind

0 minutes of yellow glare 0 minutes of green glare

Flight Path: GA, Rwy 14 Final

0 minutes of yellow glare 0 minutes of green glare

Flight Path: GA, Rwy 14 Upwind

0 minutes of yellow glare 0 minutes of green glare

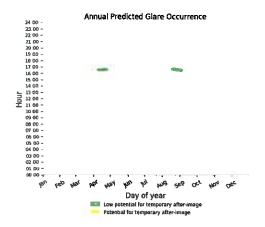
Flight Path: GA, Rwy 30 Base

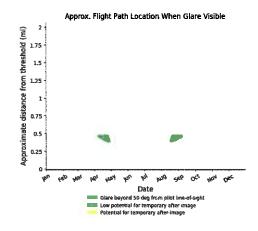
0 minutes of yellow glare 0 minutes of green glare

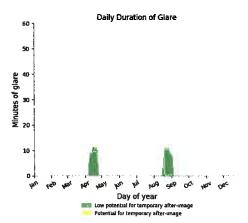
Flight Path: Ga, Rwy 30 Crosswind

0 minutes of yellow glare

310 minutes of green glare

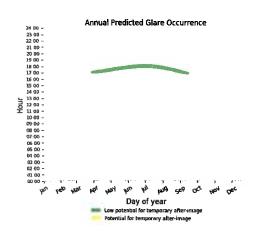


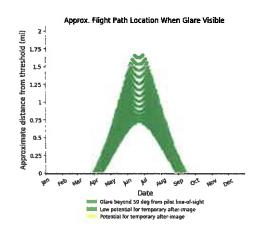


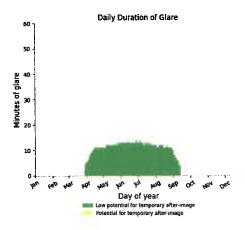


Flight Path: Ga, Rwy 30 Downwind

0 minutes of yellow glare 1888 minutes of green glare







Flight Path: GA, Rwy 30 Final

0 minutes of yellow glare 0 minutes of green glare

Flight Path: Ga, Rwy 30 Upwind

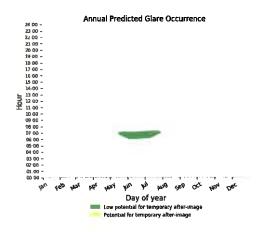
0 minutes of yellow glare 0 minutes of green glare

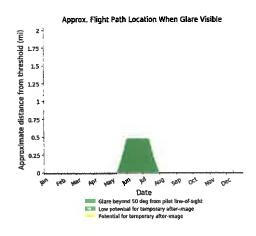
Flight Path: GA, Rwy 32 Base

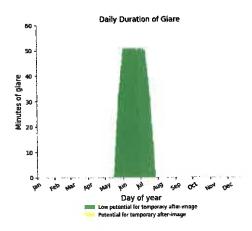
0 minutes of yellow glare 0 minutes of green glare

Flight Path: Ga, Rwy 32 Crosswind

0 minutes of yellow glare 2942 minutes of green glare







Flight Path: Ga, Rwy 32 Downwind

0 minutes of yellow glare 0 minutes of green glare

Flight Path: GA, Rwy 32 Final

0 minutes of yellow glare 0 minutes of green glare

Flight Path: GA, Rwy 32 Upwind

0 minutes of yellow glare 0 minutes of green glare

Flight Path: OHead, Rwy 14 Downwind

0 minutes of yellow glare 0 minutes of green glare

Flight Path: OHead, Rwy 14 Final

0 minutes of yellow glare 0 minutes of green glare

Flight Path: OHead, Rwy 14 Initial

0 minutes of yellow glare 0 minutes of green glare

Flight Path: OHead, Rwy 32 Downwind

0 minutes of yellow glare 0 minutes of green glare

Flight Path: OHead, Rwy 32 Final

0 minutes of yellow glare 0 minutes of green glare

Flight Path: OHead, Rwy 32 Initial

0 minutes of yellow glare 0 minutes of green glare

Flight Path: Rwy 12-Upwind

0 minutes of yellow glare 0 minutes of green glare

Point Receptor: 1-ATCT

0 minutes of yellow glare0 minutes of green glare

Assumptions

"Green" glare is glare with low potential to cause an after-image (flash blindness) when observed prior to a typical blink response time.

"Yellow" glare is glare with potential to cause an after-image (flash blindness) when observed prior to a typical blink response time.

Times associated with glare are denoted in Standard time. For Daylight Savings, add one hour.

Glare analyses do not account for physical obstructions between reflectors and receptors. This includes buildings, tree cover and geographic obstructions.

Several calculations utilize the PV array centroid, rather than the actual glare spot location, due to algorithm limitations. This may affect results for large PV footprints. Additional analyses of array sub-sections can provide additional information on expected glare.

The subtended source angle (glare spot size) is constrained by the PV array footprint size. Partitioning large arrays into smaller sections will reduce the maximum potential subtended angle, potentially impacting results if actual glare spots are larger than the sub-array size. Additional analyses of the combined area of adjacent sub-arrays can provide more information on potential glare hazards. (See previous point on related limitations.)

Glare locations displayed on receptor plots are approximate. Actual glare-spot locations may differ.

Glare vector plots are simplified representations of analysis data. Actual glare emanations and results may differ.

The glare hazard determination relies on several approximations including observer eye characteristics, angle of view, and typical blink response time. Actual results and glare occurrence may differ.

Hazard zone boundaries shown in the Glare Hazard plot are an approximation and visual aid based on aggregated research data. Actual ocular impact outcomes encompass a continuous, not discrete, spectrum.

Refer to the Help page at www.forgesolar.com/help/ for assumptions and limitations not listed here.

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NOTICE OF PUBLIC HEARING RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

www.rcaluc.org

A PUBLIC HEARING has been scheduled before the Riverside County Airport Land Use Commission (ALUC) to consider the applications described below.

Any person may submit written comments to the ALUC before the hearing or may appear and be heard in support of or opposition to the project at the time of hearing. For more information please contact <u>ALUC Planner Paul Rull at (951) 955-6893</u>. The ALUC holds hearings for local discretionary permits within the Airport Influence Area, reviewing for aeronautical safety, noise and obstructions. ALUC reviews a proposed plan or project solely to determine whether it is consistent with the applicable Airport Land Use Compatibility Plan.

The City of Moreno Valley Building and Safety Division should be contacted on non-ALUC issues. For more information please contact City of Moreno Valley Building and Safety contact Ms. Grace Espino-Salcedo at (951) 413-3451.

The proposed project application may be viewed by a prescheduled appointment and on the ALUC website www.rcaluc.org. Written comments may be submitted at the Riverside County Administrative Center, 4080 Lemon Street, 14th Floor, Riverside, California 92501, Monday through Friday from 8:00 a.m. to 3:30 p.m., or by e-mail to prull@rivco.org. Individuals with disabilities requiring reasonable modifications or accommodations, please telephone Barbara Santos at (951) 955-5132.

PLACE OF HEARING: Riverside County Administration Center

4080 Lemon Street, 1st Floor Board Chambers

Riverside California

DATE OF HEARING: May 13, 2021

TIME OF HEARING: 9:30 A.M.

Pursuant to Executive Order N-25-20, this meeting will be conducted by teleconference and at the Place of Hearing, as listed above. Public access to the meeting location will be allowed, but limited to comply with the Executive Order. Information on how to participate in the hearing will be available on the ALUC website at www.rcaluc.org

CASE DESCRIPTION:

ZAP1462MA21 – Nautilus Abrasives Blasting and Powder Coating, LLC (Representative: Renova Energy) – City of Moreno Valley Case No. BAP21-0330 (Building Permit), a proposal to construct a 7,345 square foot rooftop solar panel system on an existing industrial building on 1.0 acre located at 14451 Commerce Center Drive, northerly of Cactus Avenue, easterly of Old 215 Frontage Road, and southerly of Alessandro Boulevard (Airport Compatibility Zones B1-APZ-I and B2 of the March Air Reserve Base/Inland Port Airport Influence Area).



RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

APPLICATION FOR MAJOR LAND USE ACTION REVIEW

ALUC CASE NUMBER:	ZAP1462MAZI DATE SUBMITTED:	03/16/21
APPLICANT / REPRESENTA	ATIVE / PROPERTY OWNER CONTACT INFORMATION	
Applicant	Renova Energy	Phone Number 760-568-3413
Mailing Address	75181 Mediterranean Ave	Email permits@renovaenergy.com
	Palm Desert, CA 92211	
Representative	Renova Energy - Jamie Silva	Phone Number 760-368-8033
Mailing Address	75181 Mediterranean Ave	Email permits@renovaenergy.com
	Palm Desert, CA 92211	
Property Owner	Nautilus Abrasives Blasting and Powder Coating LLC	Phone Number 951-842-6260
Mailing Address	14451 Commerce Center Drive	Email cphilips@nautiluspowdercoating.com
	Moreno Valley, CA 92553	
LOCAL JURISDICTION AGI	ENCY	
Local Agency Name	City of Moreno Valley	Phone Number 951-413-3451
Staff Contact	gracee@moval.org GRALE BPINO - SALLEDO	Email gracee@moval.org
Mailing Address	14177 Frederick St	Case Type
	Moreno Valley, CA 92553	General Plan / Specific Plan Amendment Zoning Ordinance Amendment Subdivision Parcel Map / Tentative Tract
Local Agency Project No	BAP21-0330	Use Permit
ECCLI / IGENOS · 1 O J CONTROL	BAF21-0330	Site Plan Review/Plot Plan Other
PROJECT LOCATION		
Attach an accurately scaled n	ap showing the relationship of the project site to the airport boundary and runwa	ys
Street Address	14451 Commerce Center Drive	
	Moreno Valley, CA 92553	
Assessor's Parcel No.	297-130-062	Gross Parcel Size 1 Acre Nearest Airport and
Subdivision Name	PM 32392	distance from Air-
Lot Number	3	port March Air Reserve Base - 275
PROJECT DESCRIPTION If applicable, attach a detaile tional project description dat	ed site plan showing ground elevations, the location of structures, open spaces and	water bodies, and the heights of structures and trees; include addi-
Existing Land Use (describe)	17,000 Sq. Ft. Commercial/Industrial Building used for powde	er and liquid industrial painting and coatings.

B. B.Z.

Proposed Land Use	Renova Energy Corp. is proposing to install a 7,345 Sq. Ft. Solar Photovoltaic System on the existing roof						
(describe)	of the commercia	al building. T	his will be a ballasted	system, facing south	at a 5 degree tilt angle.		
For Residential Uses	Number of Parcels of	or Units on Site	e (exclude secondary unit	 :s)			
For Other Land Uses	Hours of Operation	0 0.0	0 pm, Monday -Frida				
(See Appendix C)	Number of People of	on Site 10	Maximum Number	85	-		
	Method of Calcula		17,000 SF Building	/ 100 sq. ft./occupar	nt = 170 people max (under CBC).		
			170 people max x 5	i0% adjustment = 85	people maximum per Appendix C.		
Height Data	Site Elevation (abov	/e mean sea lev	vel)		1555	ft.	
	Height of buildings	or structures (f	from the ground)		20' - Solar panels to add 12"	ft.	
Flight Hazards		•	acteristics which could cr other electrical or visual I				
	If yes, describe Solar Panel Glare						
				· · · · · · · · · · · · · · · · · · ·			

- A. NOTICE: Failure of an applicant to submit complete or adequate information pursuant to Sections 65940 to 65948 inclusive, of the California Government Code, MAY constitute grounds for disapproval of actions, regulations, or permits.
- B. REVIEW TIME: Estimated time for "staff level review" is approximately 30 days from date of submittal. Estimated time for "commission level review" is approximately 45 days from date of submittal to the next available commission hearing meeting.
- C. SUBMISSION PACKAGE:
 - 1. Completed ALUC Application Form
 - 1. ALUC fee payment
 - 1. Plans Package (24x36 folded) (site plans, floor plans, building elevations, grading plans, subdivision maps)
 - 1. Plans Package (8.5x11) (site plans, floor plans, building elevations, grading plans, subdivision maps, zoning ordinance/GPA/SPA text/map amendments)
 - 1. CD with digital files of the plans (pdf)
 - 1. Vicinity Map (8.5x11)
 - 1. Detailed project description
 - 1. Local jurisdiction project transmittal
 - 3. Gummed address labels for applicant/representative/property owner/local jurisdiction planner
 - 3. Gummed address labels of all surrounding property owners within a 300 foot radius of the project site. (Only required if the project is scheduled for a public hearing Commission meeting)

HYBRID INTENSITY POLICY FOR PARCEL DELIVERY FACILITIES

BACKGROUND:

Airport Land Use Commission (ALUC) staff currently reviews a project's building intensity by using the 1) Building Code method and 2) Parking Code method, as outlined in the Countywide Plan. These approved methodologies are sufficient enough for staff to perform intensity calculations on most projects. However, in the case of Parcel Delivery Facilities, these two methodologies independently are not enough to calculate the project's intensity. The specific use of Parcel Delivery Facilities, which includes the use of indoor van parking spaces, loading spaces, and queuing/stacking spaces, in addition to the regular building floor area such as warehouse, manufacturing, office, etc., requires the use of a hybrid intensity methodology that is not provided in the Countywide Plan so staff can perform more realistic intensity calculations.

A discussion with the ALUC was held on March 11, 2021, where ALUC staff was directed to bring back a Policy for the Commission's consideration. This Policy outlines ALUC staff's recommended approach to reviewing Parcel Delivery Facilities as it relates to intensity calculation.

POLICY:

When a Parcel Delivery Facility project is submitted to ALUC, ALUC staff is authorized to consider using a "hybrid intensity methodology" to calculate the project's intensity when calculating intensity by the Building Code method and Parking Code method independently are insufficient and where the project's design involves the use of indoor van parking spaces, loading spaces, and/or queuing/stacking spaces. Specifically, the "hybrid intensity methodology" will use a combination of the Building Code method for such areas as normally defined by the Building Code, such as warehouse, office and manufacturing areas, as well as the Parking Code method, which will be applied to areas inside the building identified as indoor van delivery parking, loading, queuing and stacking.

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

STAFF REPORT

ADMINISTRATIVE ITEMS

5.1 <u>Director's Approvals.</u>

A. During the period of March 16, 2021, through April 15, 2021, as authorized pursuant to Section 1.5.2(d) of the 2004 Riverside County Airport Land Use Compatibility Plan, ALUC Director Paul Rull reviewed two non-legislative cases within Zone E of the March Air Reserve Base/Inland Port Airport Influence Area.

ZAP1459MA21 (March Air Reserve Base/Inland Port Airport Zone E) pertains to County of Riverside Case No. PUP210002 (Public Use Permit), a proposal to construct a 53,466 square foot temple facility including a 4,566 square foot exhibition building, a 5,448 square foot prayer hall, a two-story 11,918 square foot auditorium, two 2,298 square foot meeting rooms, a 3,515 square foot facility office building, a 9,792 square foot Sunday School building, a 8,011 square foot dining hall, a 1,854 square foot library, and a 3,216 square foot ensuite building on 30.79 acres located southerly of Landin Lane, westerly of Cole Avenue, northerly of Markham Street, and easterly of Barton Street. The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, non-residential intensity is not restricted.

The elevation of Runway 14-32 at March Air Reserve Base/Inland Port Airport is approximately 1,535 feet above mean sea level (AMSL) at its northerly terminus. At a distance of 19,000 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with an elevation at top of roof exceeding 1,725 feet AMSL. The project site elevation is 1,684 feet AMSL, and the tallest proposed structure height is 40 feet, for a maximum top point elevation of 1,724 feet AMSL. Therefore, Federal Aviation Administration (FAA) obstruction evaluation review for height/elevation reasons is not required.

ALUC Director Paul Rull issued a determination of consistency for this project on March 24, 2021.

ZAP1460MA21 (March Air Reserve Base/Inland Port Airport Zone E) pertains to County of Riverside Case No. TPM37869 (Tentative Parcel Map), a proposal to construct a proposal to divide 4.82 gross acres located on the southwest corner of Leon Road and Bench Road into two residential lots. The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, residential density is not restricted.

Although the project is located within the March Air Reserve Base/Inland Port Airport Influence Area, the nearest runway is actually Runway 5-23 at Hemet Ryan Airport. The elevation of Runway 5-23 at Hemet Ryan Airport is approximately 1,499 feet above mean sea level (AMSL) at its southwesterly terminus. At a distance of 28,640 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with an elevation at top of roof exceeding 1,785 feet AMSL. The project site elevation is 1,714 feet AMSL. No building permits

for new structures are in process at this time, and review by the Federal Aviation Administration Obstruction Evaluation Services (FAA OES) is not a prerequisite to land division. Therefore, FAA OES review for height/elevation reasons was not required.

ALUC Director Paul Rull issued a determination of consistency for this project on April 1, 2021.

B. Additionally, as authorized pursuant to ALUC Resolution No. 2015-01, as extended by Resolution Nos. 2016-02 and 2018-02, ALUC Director Paul Rull reviewed one legislative case within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area and issued determination of consistency.

ZAP1463MA21 (March Air Reserve Base/Inland Port Airport, Zone E) pertains to City of Moreno Valley Case Nos. PEN20-0095 (General Plan Amendment), PEN20-0096 (Change of Zone), PEN21-0066 (Conditional Use Permit), a proposal to develop 108 detached condominium units on 17.3 acres within a 32.7 acre parcel located easterly of Gernert Road, northerly of Jennings Court, westerly of Hilmer Court, and southerly of the Box Springs Mountain Reserve Park. The applicant also proposes to amend 17.3 acres of the site's General Plan land use designation from Residential: Max 2 DU/AC to Residential: Max 10 DU/AC, and to amend the remaining 15.4 acres of the site's General Plan land use designation from Hillside Residential to Open Space, as well as a change the zoning of 17.3 acres from R2-2DU/AC to RS10-10DU/AC, and a change to the zoning of the remaining 15.4 acre portion of the site from Hillside Residential to Open Space. The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, residential density and non-residential intensity are not restricted.

The elevation of Runway 14-32 at March Air Reserve Base/Inland Port Airport is approximately 1,535 feet above mean sea level (AMSL) at its northerly terminus. At a distance of 23,300 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with an elevation at top of roof exceeding 1,768 feet AMSL. The project site elevation is 1,680 feet AMSL, and the tallest proposed structure is 35 feet, for a maximum top point elevation of 1,715 feet AMSL. There are no proposed changes to the existing building height. Therefore, Federal Aviation Administration (FAA) obstruction evaluation review for height/elevation reasons is not required.

ALUC Director Paul Rull issued a determination of consistency for this project on April 14, 2021.

C. Additionally, as authorized pursuant to ALUC Resolution No. 2020-02, ALUC Director Paul Rull reviewed one non-legislative case within Airport Compatibility Zone D of the French Valley Airport Influence Area and issued determination of consistency.

ZAP1110FV21 (French Valley Airport Influence Area Zone D) pertains to County of Riverside Case No. PP26344 (Plot Plan Amendment), a proposal to revise the internal floor plan for a 34,000 square foot Fitness Center (Building #11) as part of a 132,568 square foot retail commercial center on 21.16 acres which was previously found consistent under ZAP1087FV19, located on the northeast corner of Thompson Road and Highway 79 Winchester Road. The proposed changes to the internal floor area of the 34,000 square foot fitness center building includes: 16,748 square feet of exercise room area, 1,020 square feet of office area, 105 square feet of reception/lounge area, 85 square feet of retail area, 2,146 square feet of daycare area, 1,308 square feet of locker room area, 1,916 square feet of pool area, 370 square feet of mechanical equipment room area, and 626 square feet of storage area. The proposed revisions result in a total occupancy

of 484 people, which results in an average intensity of 132 people per acre, and a single acre intensity of 484 people, both of which are consistent with Compatibility Zone D average and single acre criterion of 150 and 518 respectively. (Note, a 518 occupancy limit was based on the single-acre risk reduction measures that was granted to the original project for this building, warranting a 15% bonus to the normal single acre criterion, increasing it from 450 people to 518 people). There are no changes to the footprint size of the building. The proposed revisions does not result in a change of occupancy (484 people) and intensity (132 people average acre and 518 people single acre) from the original project ZAP1087FV19.

ALUC Director Paul Rull issued a determination of consistency for this project on April 7, 2021.

5.2 <u>Update March Air Reserve Base Compatibility Use Study (CUS)</u>
Presentation by Project Director Simon Housman or his designee.

Y:\ALUC Administrative Items\Admin. 2021\ADmin Item 05-13-21.doc

AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY



Mr. Timothy Wheeler, Project Planner Riverside County Planning Division 4080 Lemon Street, 12th Floor Riverside CA 92501

Russell Betts Desert Hot Springs VICE CHAIR Steven Stewart

CHAIR

RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW -DIRECTOR'S DETERMINATION

COMMISSIONERS

Palm Springs

ZAP1459MA21 File No.:

Arthur Butler Riverside

PUP210002 (Public Use Permit)

APN:

Related File No.:

266-320-025

John Lyon

Compatibility Zone E Airport Zone:

Riverside Steve Manoe

Lake Eislnore

Dear Mr. Wheeler:

Richard Stewart Morenc Valley

Gary Youmans Temecula

STAFF

Director Paul Rui

Simon A. Housman Paul Rull Barbara Santos

County Administrative Center 4080 Lerron St., 14th Floor. Resealch, CA92501 (951) 955-6132

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed County of Riverside Case No. PUP 210002 (Public Use Permit), a proposal to construct a 53,466 square foot temple facility including a 4,566 square foot exhibition building, a 5,448 square foot prayer hall, a two-story 11,918 square foot auditorium, two 2,298 square foot meeting rooms, a 3,515 square foot facility office building, a 9,792 square foot Sunday School building, a 8,011 square foot dining hall, a 1,854 square foot library, and a 3,216 square foot ensuite building on 30.79 acres located southerly of Landin Lane, westerly of Cole Avenue, northerly of Markham Street, and easterly of Barton Street.

The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, non-residential intensity is not restricted.

The elevation of Runway 14-32 at March Air Reserve Base/Inland Port Airport is approximately 1,535 feet above mean sea level (AMSL) at its northerly terminus. At a distance of 19,000 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with an elevation at top of roof exceeding 1,725 feet AMSL. The project site elevation is 1,684 feet AMSL, and the tallest proposed structure height is 40 feet, for a maximum top point elevation of 1,724 feet AMSL. Therefore, Federal Aviation Administration (FAA) obstruction evaluation review for height/elevation reasons is not required.

As ALUC Director, I hereby find the above-referenced project CONSISTENT with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, provided that the County of Riverside applies the following recommended conditions:

AIRPORT LAND USE COMMISSION

CONDITIONS:

- Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
- 2. The following uses/activities are not included in the proposed project and shall be prohibited at this site.
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Hazards to flight.
- 3. The attached notice shall be provided to all prospective purchasers of the property and tenants of the building.
- 4. The maximum height of the building, including all roof-mounted equipment, if any, shall be limited to 40 feet, and the maximum top point elevation shall not exceed 1,725 feet above mean sea level unless a "Determination of No Hazard to Air Navigation" letter authorizing a higher top point elevation has been issued by the Federal Aviation Administration Obstruction Evaluation Service.

If you have any questions, please feel free to contact me at (951) 955-6893.

Sincerely,

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

Paul Rull, ALUC Director

AIRPORT LAND USE COMMISSION

Attachments: Notice of Airport in Vicinity

cc: Robert Ritner (applicant)

Berkin Construction c/o Richard Chou (representative)

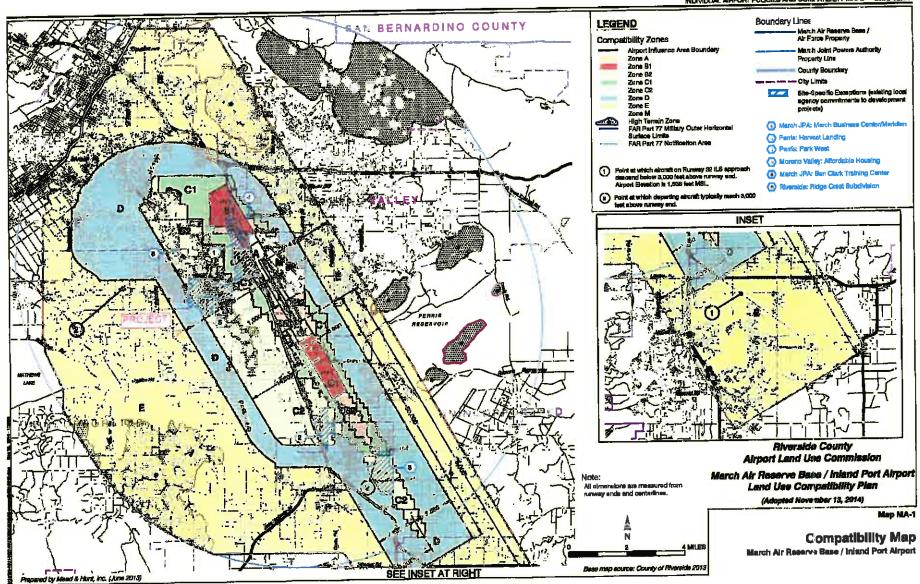
Chong Huo Tong Moral Association USA, Inc. (property owner)
Gary Gosliga, Airport Manager, March Inland Port Airport Authority
Doug Waters, Deputy Base Civil Engineer, March Air Reserve Base

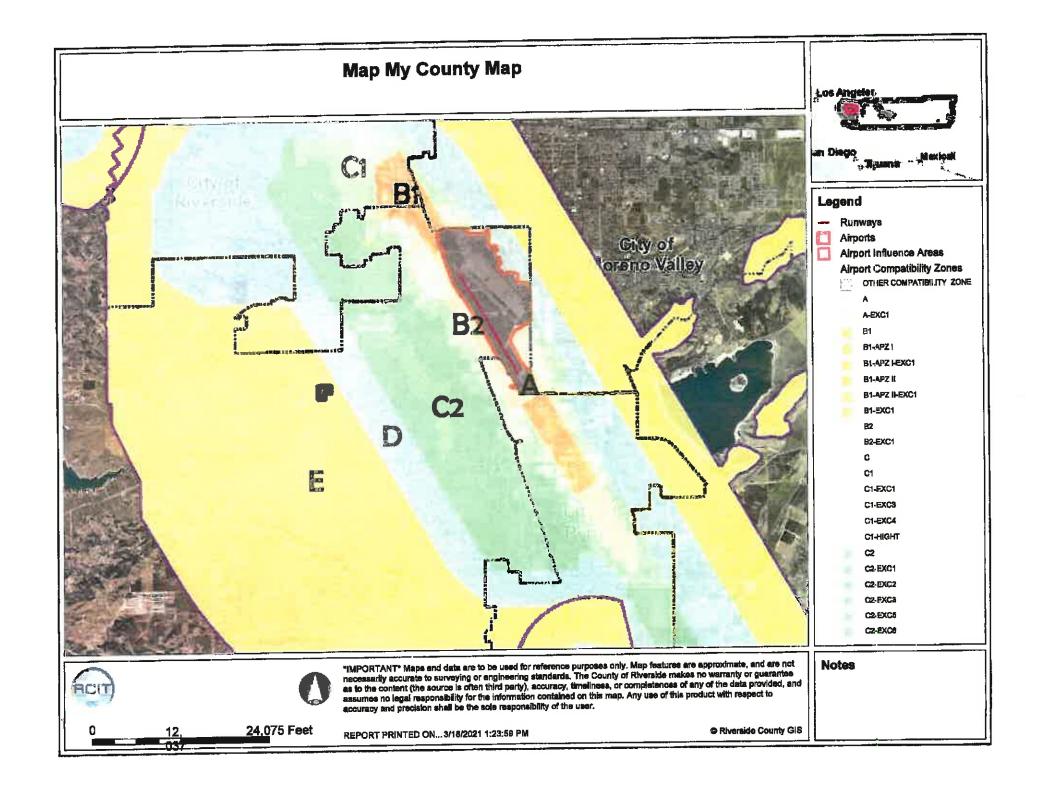
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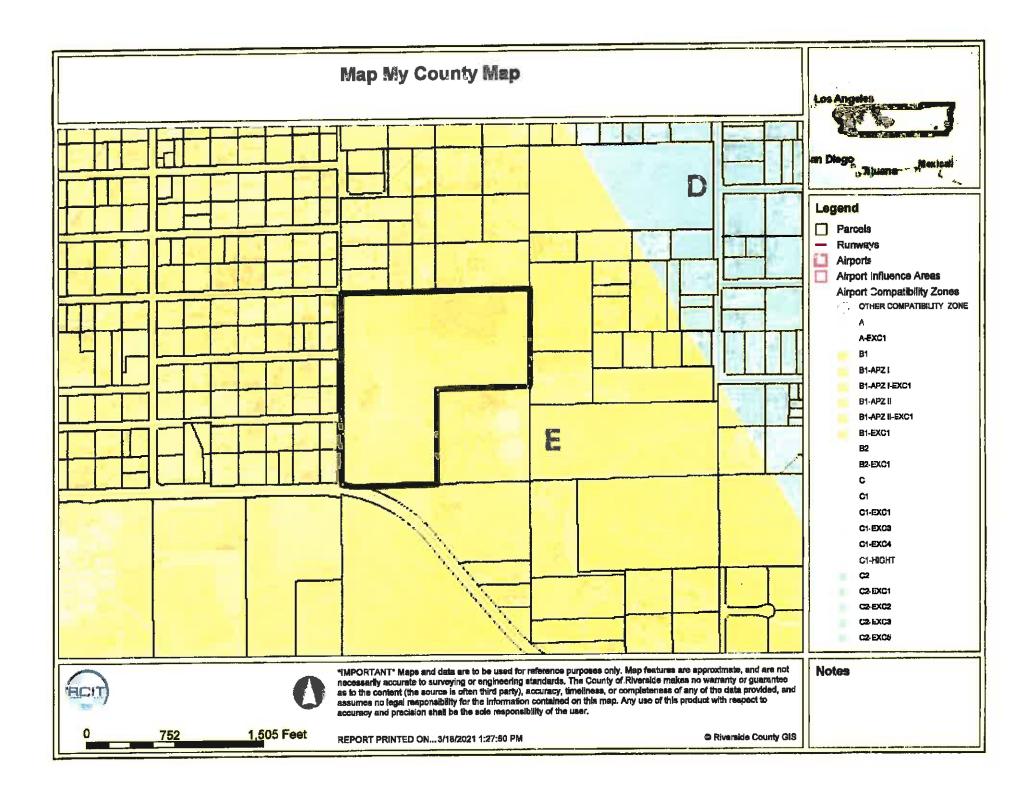
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NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to vou. Business & Professions Code Section 11010 (b)







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 Blueline Streams
- City Areas
 World Street Map





"IMPORTANT" Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the Information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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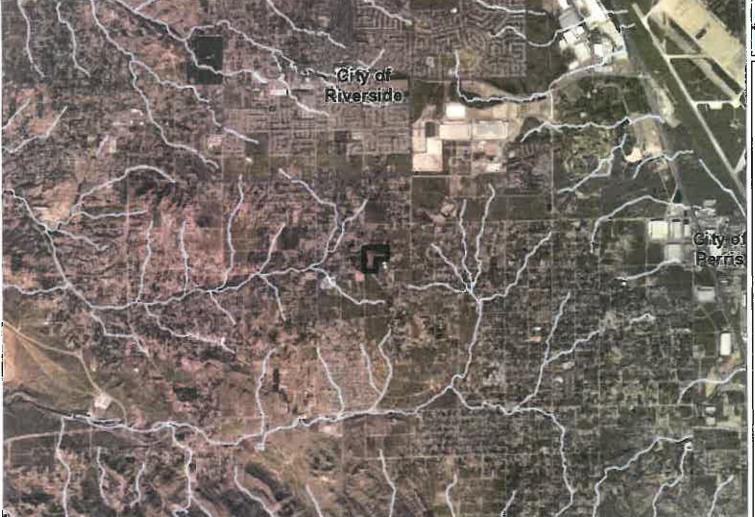
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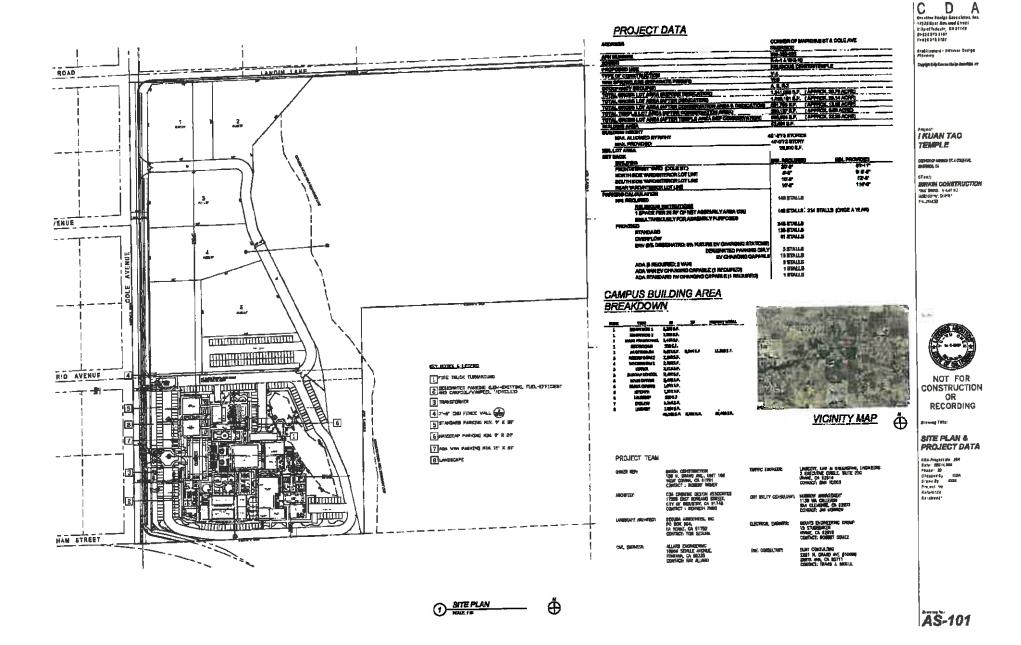
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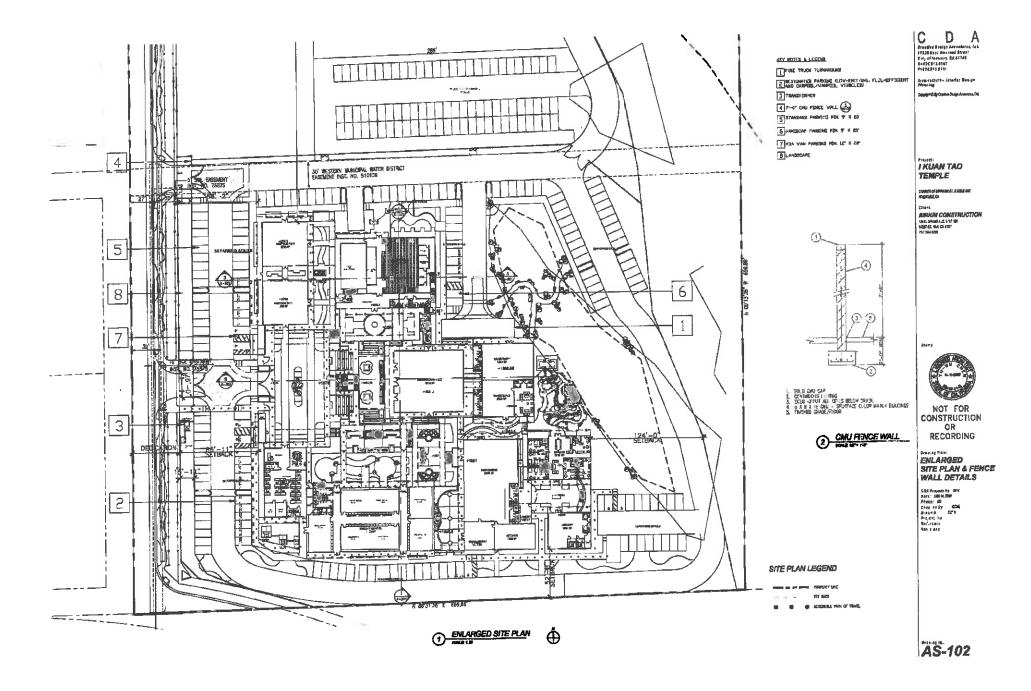
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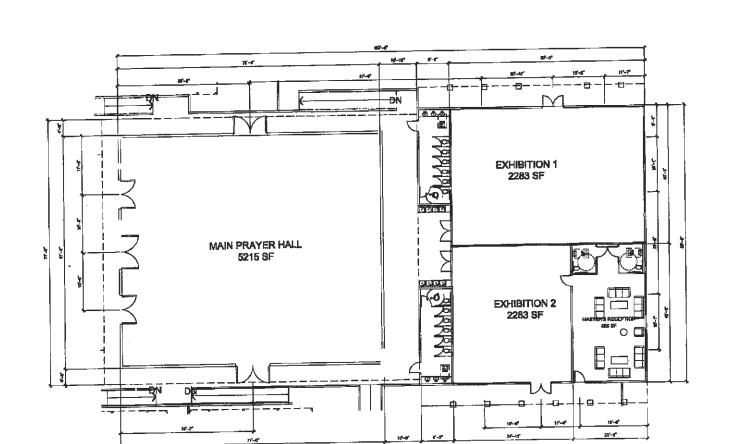


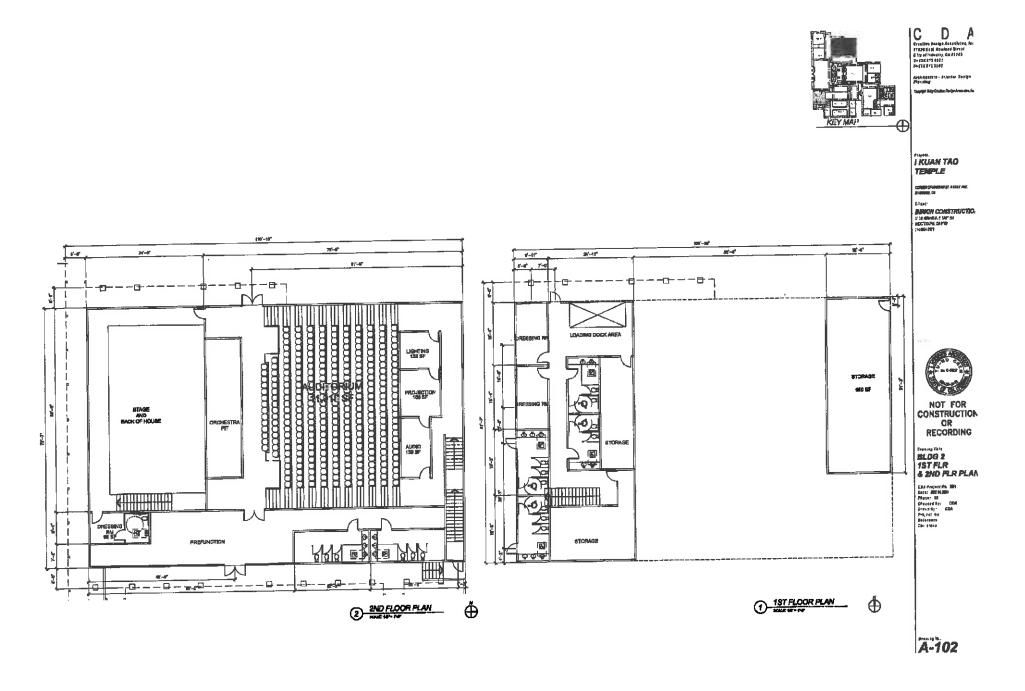
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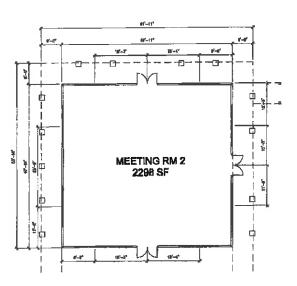
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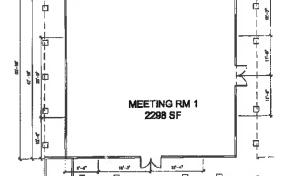
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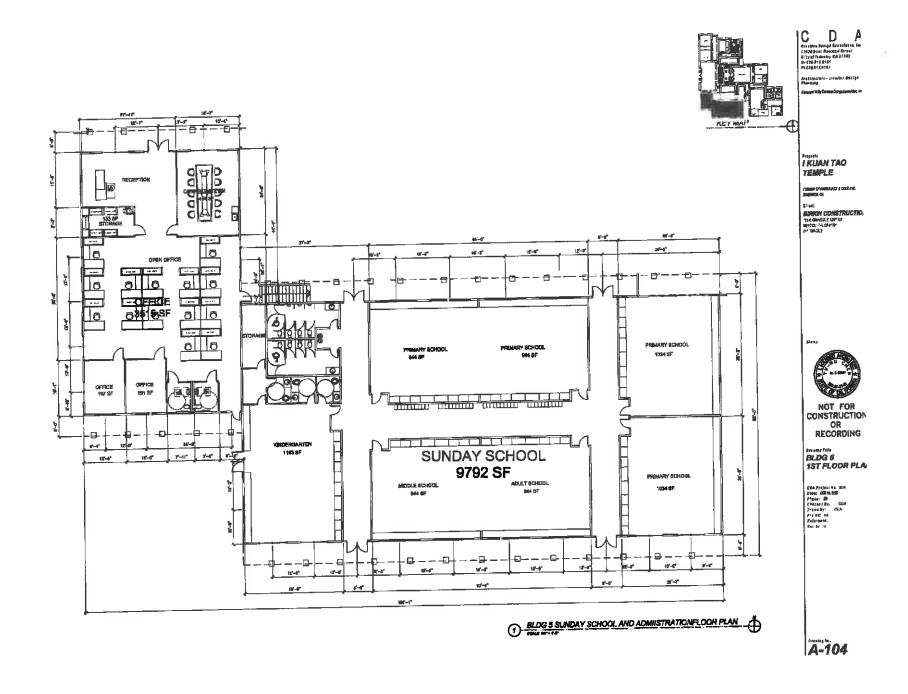


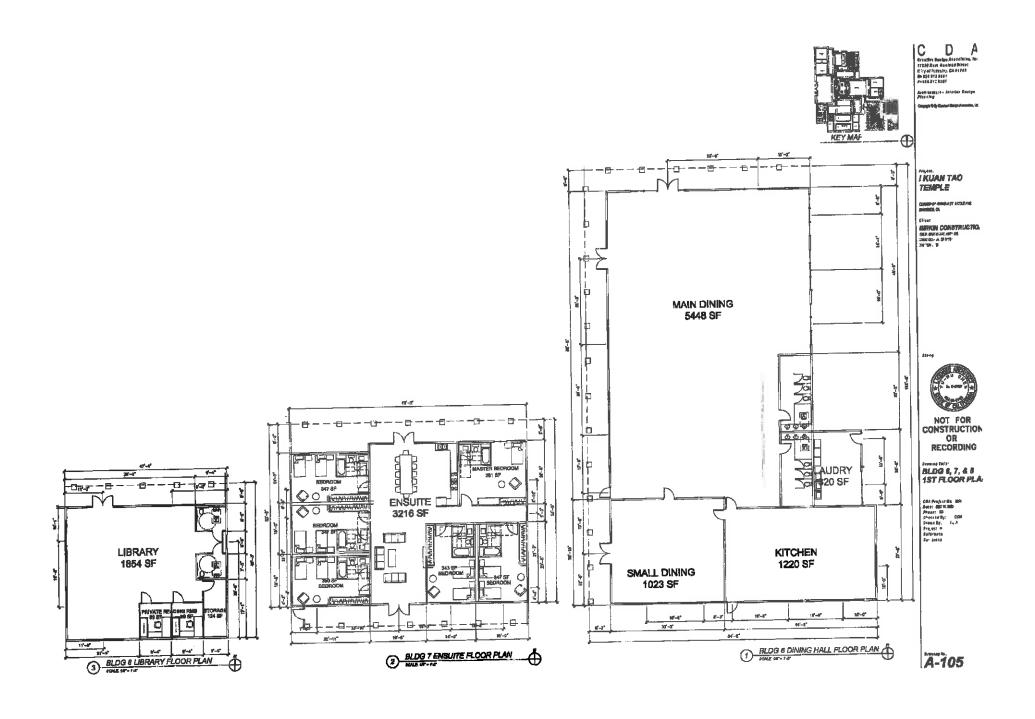
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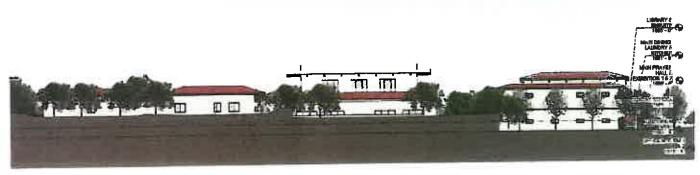


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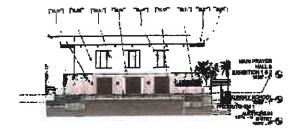
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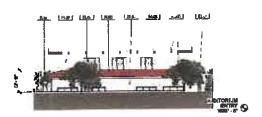
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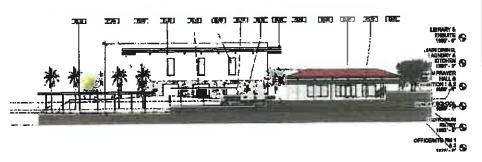
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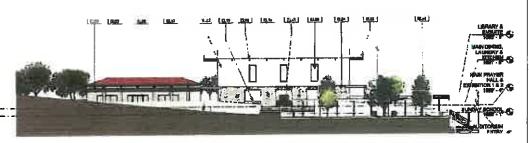
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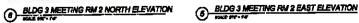








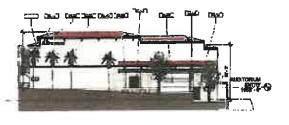


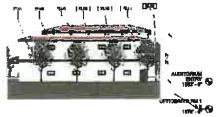




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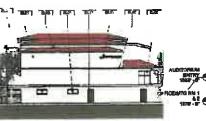
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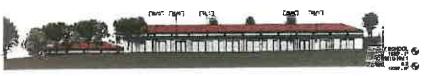
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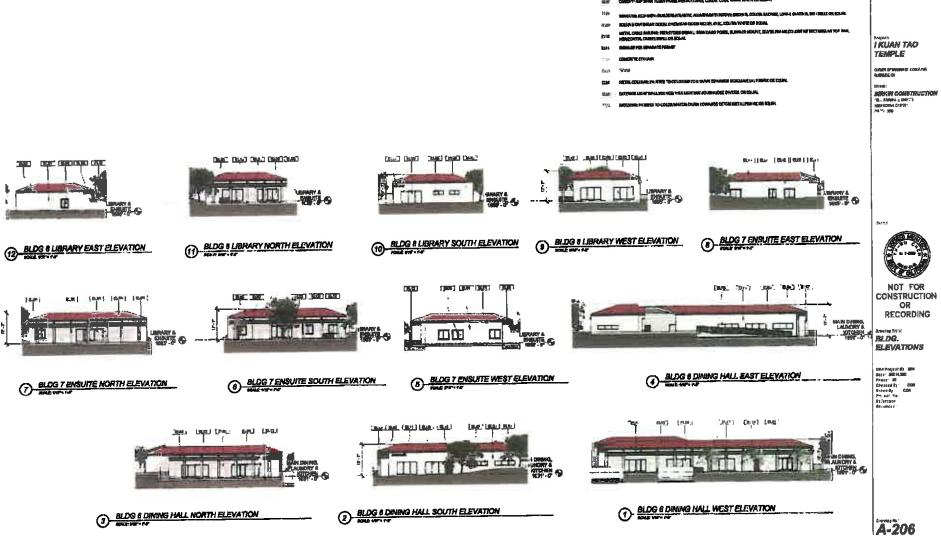
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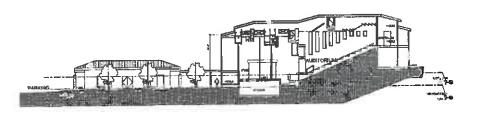


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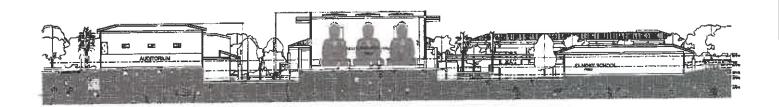
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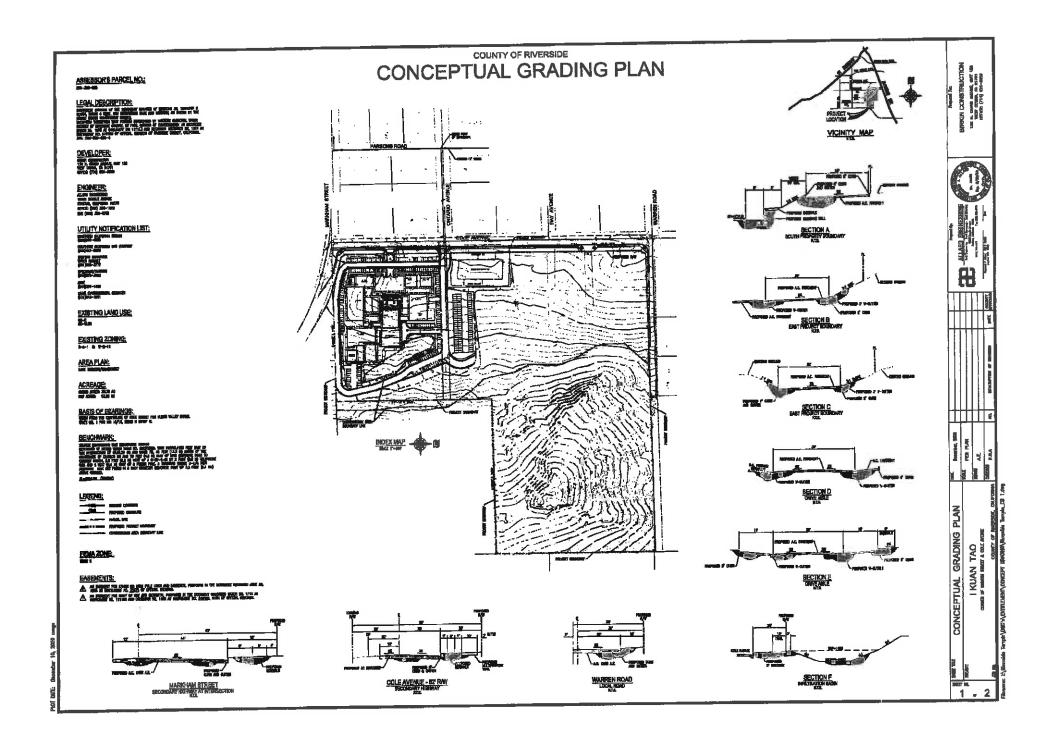


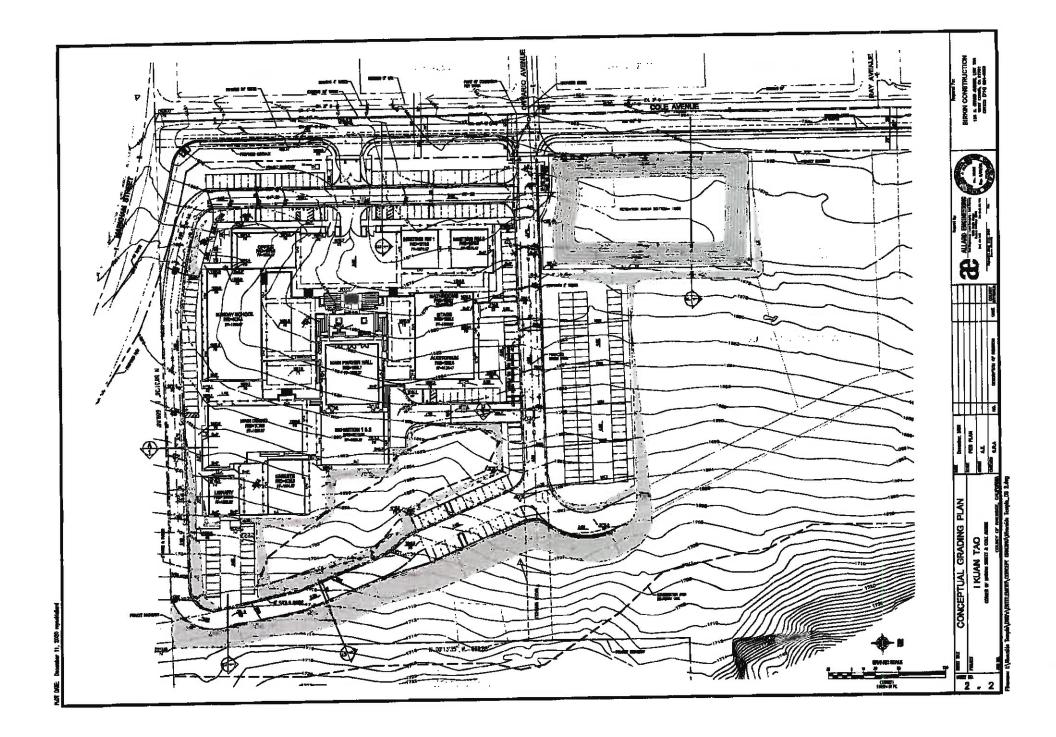
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AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY



CHAIR

April 1, 2021

Rob Gonzalez, Project Planner Riverside County Planning Division 4080 Lemon Street, 12th Floor Riverside CA 92501

Related File No.:

VICE CHAIR Steven Stewart Palm Springs

Russell Reffs

RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW = DIRECTOR'S DETERMINATION

COMPLESSIONERS

File No.: ZAP1460MA21

Arthur Butler Riverside TPM37869 (Tentative Parcel Map)

APN:

457-250-051

John Lyon Riverside Airport Zone: Compatibility Zone E

Steve Manos Lake Elsinore

Dear Mr. Gonzalez:

Richard Stewart

Moreno Valley

Gary Youmans Temecula

STAFF

Director Paul Ruli

Simon A. Housman Daniel Zerda Barbara Santos

County Administrative Center 4080 Lerron St., 14th Roor. Riverside, CA 92501 (951) 955-5132

www.rcaluc.org

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to Policy 1.5.2(d) of the Countywide Policies of the 2004 Riverside County Airport Land Use Compatibility Plan, staff reviewed County of Riverside Case No. TPM37869 (Tentative Parcel Map No. 37869), a proposal to divide 4.82 gross acres located on the southwest corner of Leon Road and Bench Road into two residential lots.

The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, residential density is not restricted.

Although the project is located within the March Air Reserve Base/Inland Port Airport Influence Area, the nearest runway is actually Runway 5-23 at Hemet Ryan Airport. The elevation of Runway 5-23 at Hemet Ryan Airport is approximately 1,499 feet above mean sea level (AMSL) at its southwesterly terminus. At a distance of 28,640 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with an elevation at top of roof exceeding 1,785 feet AMSL. The project site elevation is 1,714 feet AMSL. No building permits for new structures are in process at this time, and review by the Federal Aviation Administration Obstruction Evaluation Services (FAA OES) is not a prerequisite to land division. Therefore, FAA OES review for height/elevation reasons was not required.

As ALUC Director, I hereby find the above-referenced project **CONSISTENT** with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, provided that the County of Riverside applies the following recommended conditions:

CONDITIONS:

1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be

AIRPORT LAND USE COMMISSION

downward facing.

- 2. The following uses/activities are not included in the proposed project and shall be prohibited at this site.
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, production of cereal grains, sunflower, and row crops, composting operations, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Hazards to flight.
- 3. The attached notice shall be provided to all potential purchasers, lessees, and/or tenants of the property.
- 4. Any proposed detention basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the detention basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at <u>RCALUC.ORG</u> which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the

AIRPORT LAND USE COMMISSION

name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

If you have any questions, please contact me at (951) 955-6893

Sincerely,

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

Paul Rull, ALUC Director

Attachments: Notice of Airport in Vicinity

cc: Procopio and Hermelinda Jimenez (applicant/property owner)

Daniel Jimenez (representative)

Gary Gosliga, Airport Manager, March Inland Port Airport Authority Doug Waters, Deputy Base Civil Engineer, March Air Reserve Base

ALUC Case File

Y:\AIRPORT CASE FILES\March\ZAP1460MA21\ZAP1460MA21.LTR.doc

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b)

NOTICE

THERE IS AN AIRPORT NEARBY.

THIS STORM WATER BASIN IS DESIGNED TO HOLD

STORM WATER FOR ONLY 48 HOURS AND

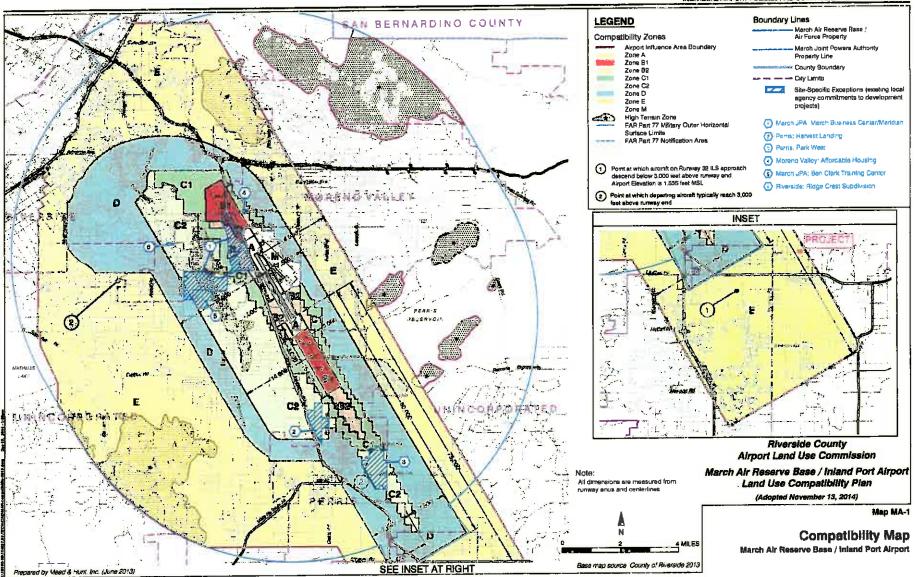
NOT TO ATTRACT BIRDS

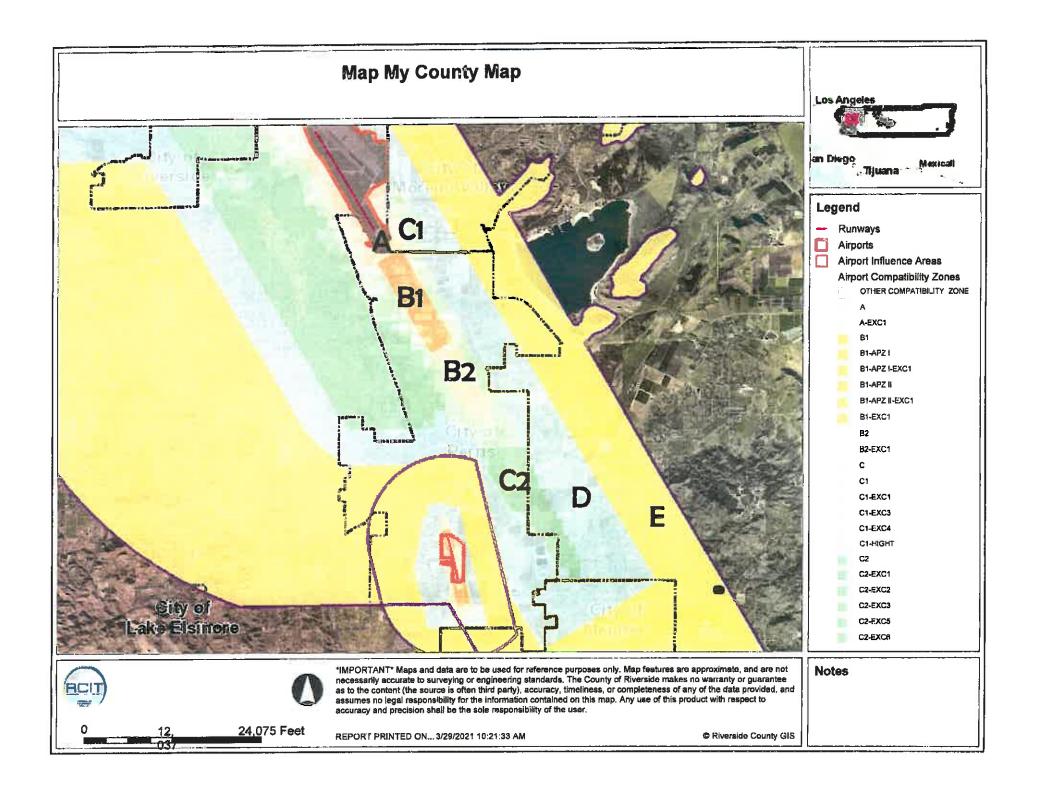
PROPER MAINTENANCE IS NECESSARY TO AVOID
BIRD STRIKES

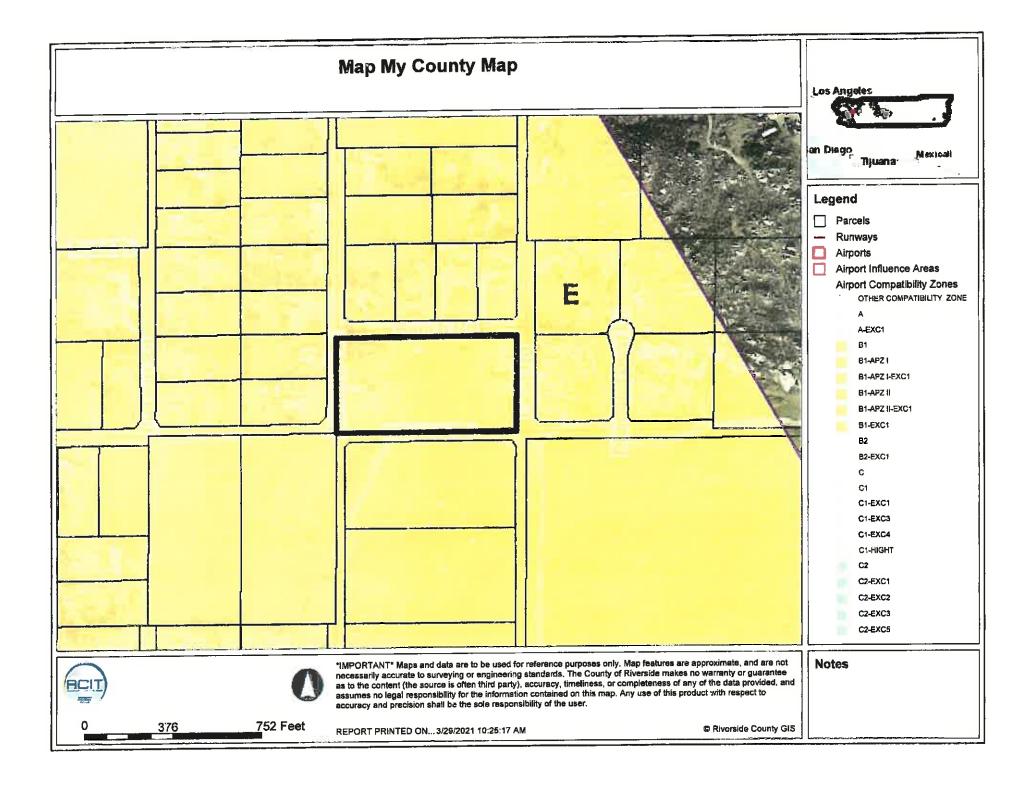


F T	HIS	BASIN	IS	OVERGROWN,	PLEASE	CONTACT:
-----	-----	-------	----	------------	--------	-----------------

		ii
Name:	Phone:	











Legend

- Parcels
 County Centerline Names
- County Centerlines
 Blueline Streams
 City Areas
 World Street Map





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752 Feet

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Notes





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County Centerlines
 Blueline Streams
 City Areas
 World Street Map





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County Centerlines Blueline Streams City Areas World Street Map





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3,009 Feet

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Legend

Parcels

 County Centerlines **Blueline Streams** City Areas World Street Map





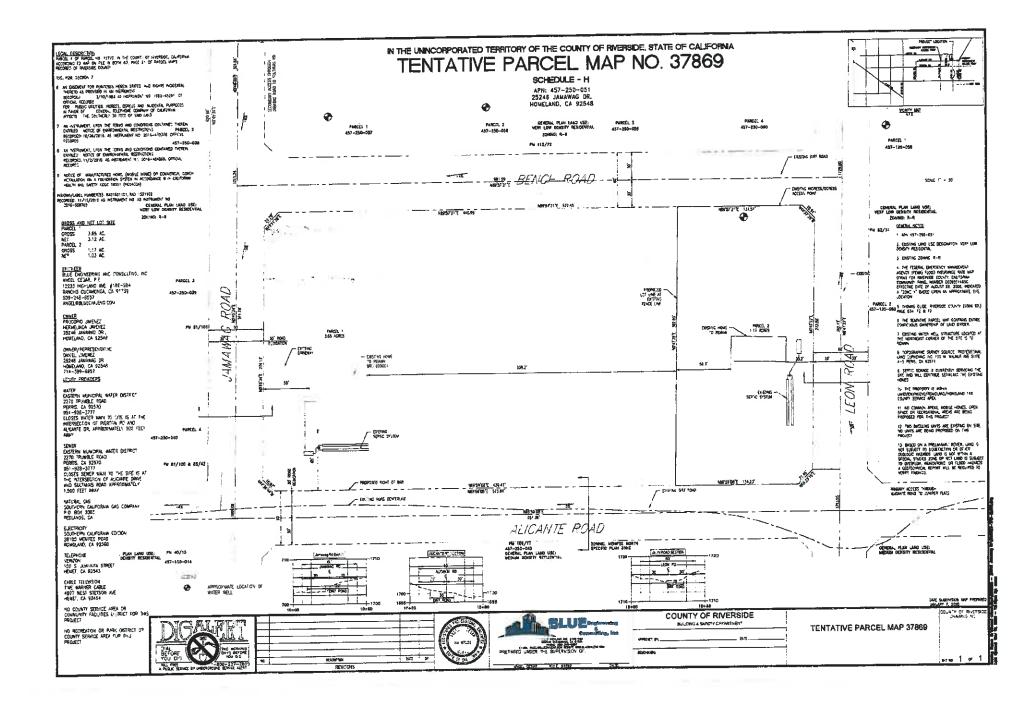
IMPORTANT Maps and data are to be used for reference purposes only. Map features are approximate, and are not necessarily accurate to surveying or engineering standards. The County of Riverside makes no warranty or guarantee as to the content (the source is often third party), accuracy, timeliness, or completeness of any of the data provided, and assumes no legal responsibility for the information contained on this map. Any use of this product with respect to accuracy and precision shall be the sole responsibility of the user.

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AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY



Mr. Sean Kelleher, Project Planner City of Moreno Valley Planning Department 14177 Frederick Street

CHAIR Steven Stewart Palm Springs

Moreno Valley CA 92552

VICE CHAIR Steve Magos Lake Eistnore

RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW – DIRECTOR'S DETERMINATION

CONTESSIONERS

File No.: ZAP1463MA21

Arthur Butler Riverside PEN20-0095 (General Plan Amendment), PEN20-0096 (Change

of Zone), PEN21-0066 (Conditional Use Permit)

John Lyon Riverside 256-150-001

Airport Zone:

Related File No.:

Compatibility Zone E

Russell Betts Desert Hot Springs

_{swart} | Dear

Richard Stewart Moreno Valley

Gary Youmana Temecula

STAFF

Director Paul Ruiz

Simon A. Housman Danlei Zerda Barbara Santos

County Administrative Center 4080 Lemon St., 14th Floor. Physiology CA 92501 (951) 925-5132 Dear Mr. Kelleher:

APN:

Under the delegation of the Riverside County Airport Land Use Commission (ALUC) pursuant to ALUC Resolution No.15-01 (as adopted on August 13, 2015), and pursuant to Policy 1.5.2(d) staff reviewed City of Moreno Valley Case Nos. PEN20-0095 (General Plan Amendment), PEN20-0096 (Change of Zone), PEN21-0066 (Conditional Use Permit), a proposal to develop 108 detached condominium units on 17.3 acres within a 32.7 acre parcel located easterly of Gernert Road, northerly of Jennings Court, westerly of Hilmer Court, and southerly of the Box Springs Mountain Reserve Park. The applicant also proposes to amend 17.3 acres of the site's General Plan land use designation from Residential: Max 2 DU/AC to Residential: Max 10 DU/AC, and to amend the remaining 15.4 acres of the site's General Plan land use designation from Hillside Residential to Open Space, as well as a change the zoning of 17.3 acres from R2-2DU/AC to RS10-10DU/AC, and a change to the zoning of the remaining 15.4 acre portion of the site from Hillside Residential to Open Space.

www.cdir.co

The site is located within Airport Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Influence Area (AIA). Within Compatibility Zone E of the March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, residential density and non-residential intensity are not restricted.

The elevation of Runway 14-32 at March Air Reserve Base/Inland Port Airport is approximately 1,535 feet above mean sea level (AMSL) at its northerly terminus. At a distance of 23,300 feet from the project to the nearest point on the runway, Federal Aviation Administration Obstruction Evaluation Service (FAA OES) review would be required for any structures with an elevation at top of roof exceeding 1,768 feet AMSL. The project site elevation is 1,680 feet AMSL, and the tallest proposed structure is 35 feet, for a maximum top point elevation of 1,715 feet AMSL. There are no proposed changes to the existing building height. Therefore, Federal Aviation Administration (FAA) obstruction evaluation review for height/elevation reasons is not required.

AIRPORT LAND USE COMMISSION

As ALUC Director, I hereby find the above-referenced project <u>CONSISTENT</u> with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, provided that the City of Moreno Valley applies the following recommended conditions:

CONDITIONS:

- 1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
- 2. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
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 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Other Hazards to flight.
- 4. The attached "Notice of Airport in Vicinity" shall be provided to all prospective purchasers and occupants of the property.
- 5. Any proposed detention basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the detention basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the detention basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the detention basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available

AIRPORT LAND USE COMMISSION

at <u>RCALUC.ORG</u> which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

If you have any questions, please contact me at (951) 955-6893.

Sincerely,

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

Paul Rull, ALUC Director

Attachments: Notice of Airport in Vicinity

cc: United Engineering Group (applicant)
Jason Ackerman (representative)

Shizao Zheng (property owner)

Gary Gosliga, Airport Manager, March Inland Port Airport Authority Doug Waters, Deputy Base Civil Engineer, March Air Reserve Base

ALUC Case File

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NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b)

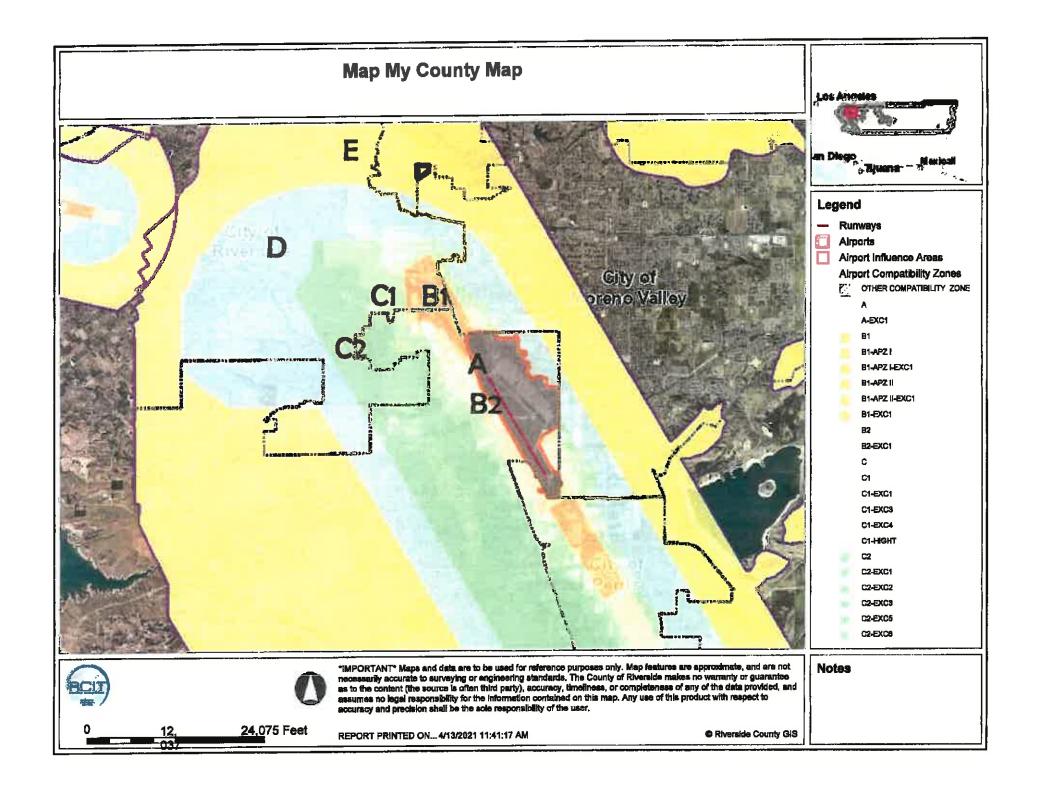
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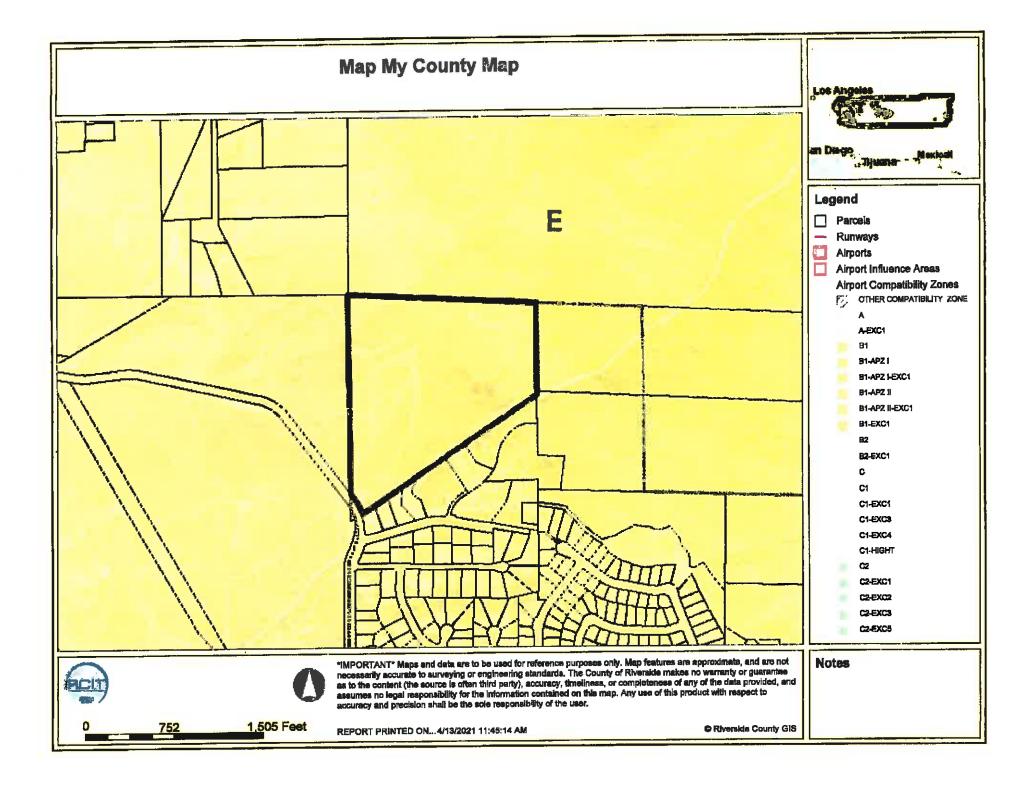
Prepared by Mead & Hunt, Inc. (June 2019)

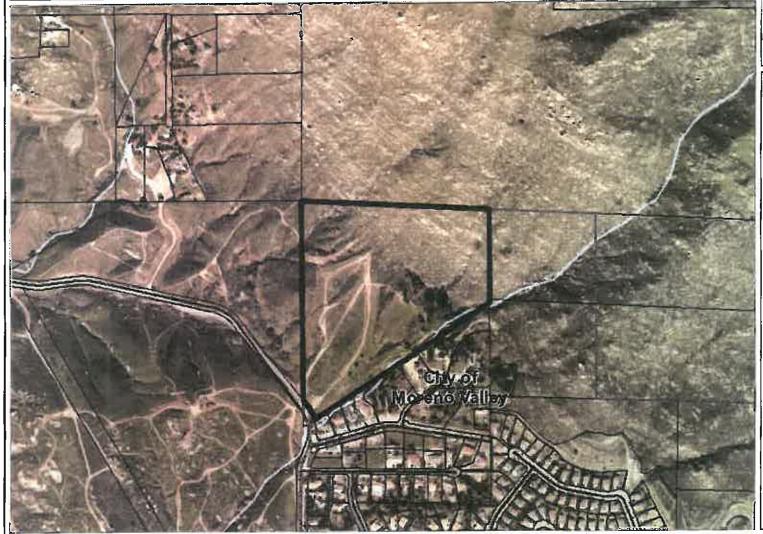
map ma-1

Compatibility Map Merch Air Reserve Bess / Inland Port Airport

Base map source; County of Favorable 2018









Legend

- Parcels
- -- County Centerlines
 Blueline Streams
- City Areas
 World Street Map





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Riverside County GIS

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 Blueline Streams
- City Areas
 World Street Map





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Notes





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Parcels

County Centerline Names

 County Centerlines **Blueline Streams**

City Areas

World Street Map



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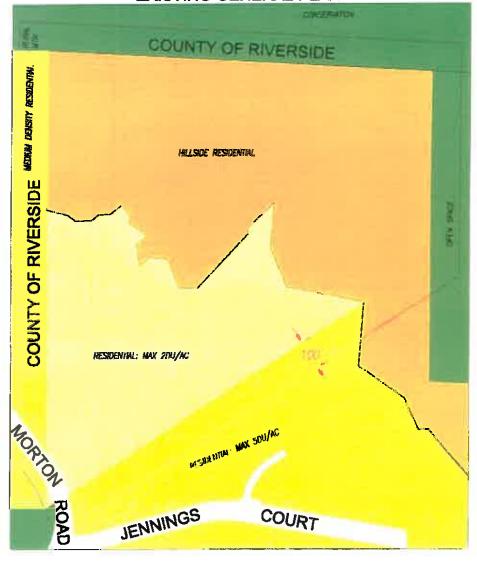
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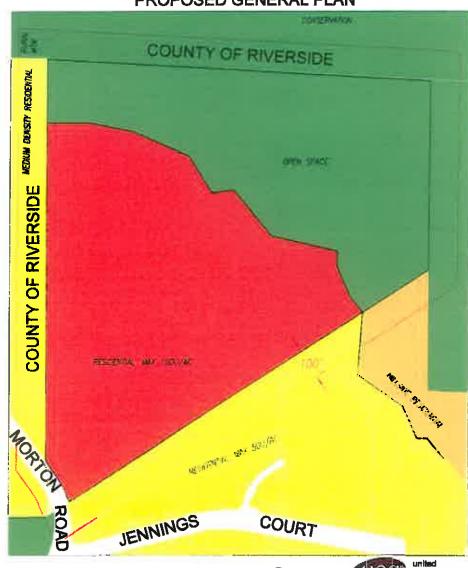
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Riverside County GIS

EXISTING GENERAL PLAN

PROPOSED GENERAL PLAN





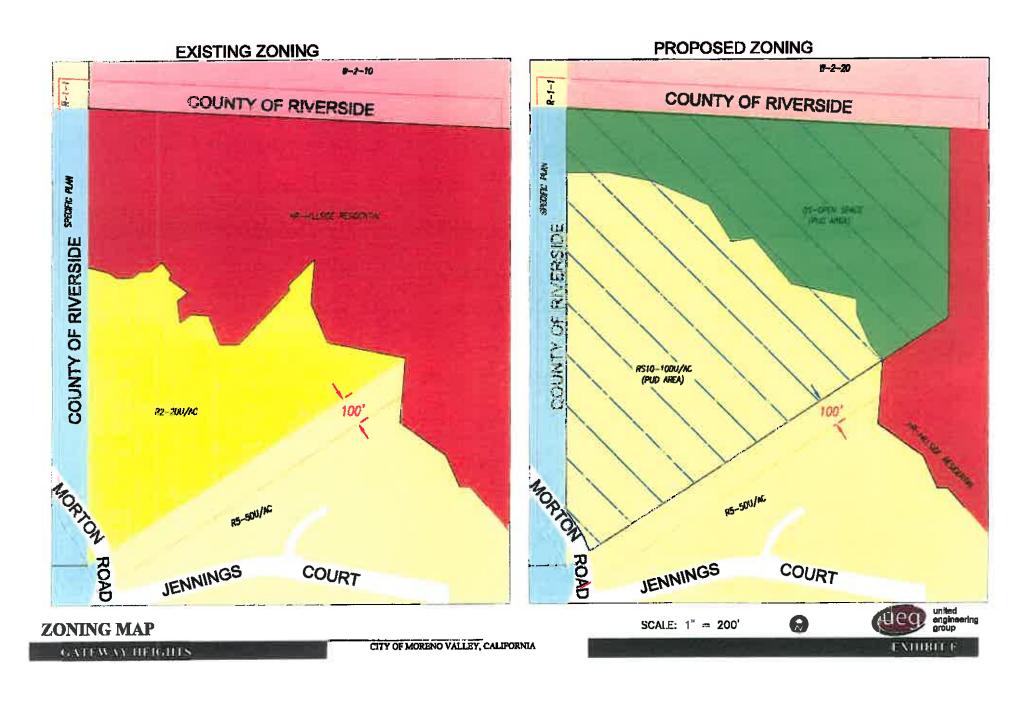
GENERAL PLAN MAP

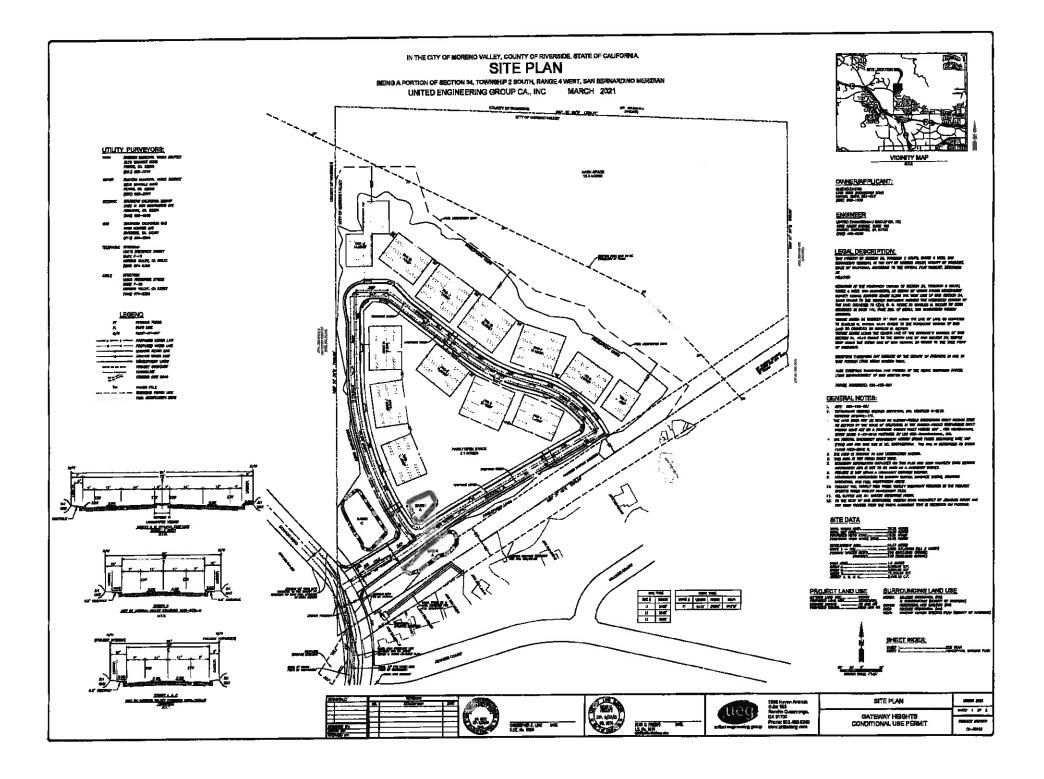
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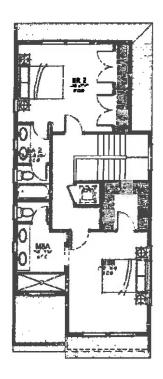


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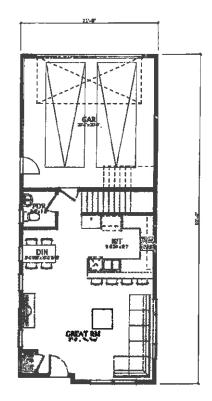








1 PLAN 1 SECOND FLOOR 785 sq ft



FIRST FLOOR 615 sq ft

TOTAL 1400 sq ft

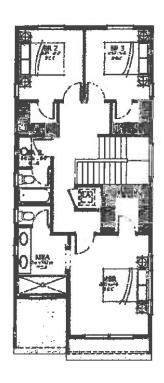
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Name Colors	NA 92614-880	

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PLAN 1

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2 PLAN 2 SECOND FLOOR 885 sq ft

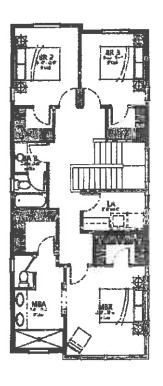
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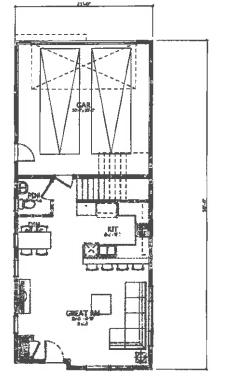
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Henghou Group 177 E. Cabrudo BLVD 5ts. 200 Presdens, CA 91105	Gateway Heights Moveno Valley, USA

PLAN 2

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3 PLAN 3 SECOND FLOOR 987 sq R

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∕ €\	PLAN 3 3 MORDON, 2.5 MARG	FOTAL	1602 sq ft
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Henghou Group 177 E. Celerado BLVD, Sta. 200 Pasadena, CA 81105 Gateway Heights Moreno Vellay, USA	_
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PLAN 1

A-31



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AIRPORT LAND USE COMMISSION RIVERSIDE COUNTY



April 7, 2021

Mr. Russell Brady, Principal Ptanner Riverside County Planning Division 4080 Lemon Street, 12th Floor Riverside CA 92501

Related File No.:

CHAIR Russell Batts Desert Hot Springs

> VICE CHAIR Steven Stewart Palm Springs

RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW – DIRECTOR'S DETERMINATION

COMMISSIONERS

File No.: ZAP1110FV21

Arthur Butler Riverside PP26344 (Plot Plan Amendment)

APN:

480-170-019

John Lyon

Airport Zone: Zone D

Riverside Steve Manos

Dear Mr. Brady:

Lake Eislnore Richard Stewart

Moreno Valley

Gary Youmana Temecuia

STAFF

Director Paul Ruii

Paul Ruil

Simon Housman Daniel Zerda Barbara Santos

County Administrative Center 4090 Lerron St., 14th Roos. Flueraids, CA 92501 (951) 955-5132

Alekasijeod

As authorized by the Riverside County Airport Land Use Commission (ALUC) pursuant to its Resolution No. 2020-02, as ALUC Director, I have reviewed County of Riverside Case No. PP26344 (Plot Plan Amendment), a proposal to revise the internal floor plan for a 34,000 square foot Fitness Center (Building #11) as part of a 132,568 square foot retail commercial center on 21.16 acres which was previously found consistent under ZAP1087FV19, located on the northeast corner of Thompson Road and Highway 79 Winchester Road.

The proposed changes to the internal floor area of the 34,000 square foot fitness center building includes:

- 16,748 square feet of exercise room area,
- 1,020 square feet of office area,
- 105 square feet of reception/lounge area.
- 85 square feet of retail area,
- 2,146 square feet of daycare area.
- 1,308 square feet of locker room area,
- 1,916 square feet of pool area,
- 370 square feet of mechanical equipment room area, and
- 626 square feet of storage area.

The proposed revisions result in a total occupancy of 484 people, which results in an average intensity of 132 people per acre, and a single acre intensity of 484 people, both of which are consistent with Compatibility Zone D average and single acre criterion of 150 and 518 respectively. (Note, a 518 occupancy limit was based on the single-acre risk reduction measures that was granted to the original project for this building, warranting a 15% bonus to the normal single acre criterion, increasing it from 450 people to 518 people).

There are no changes to the footprint size of the building. The proposed revisions does not result in a change of occupancy (484 people) and intensity (132 people average acre and 518 people

AIRPORT LAND USE COMMISSION

single acre) from the original project ZAP1087FV19.

As ALUC Director, I hereby find the above-referenced project <u>CONSISTENT</u> with the 2007 French Valley Airport Land Use Compatibility Plan, as amended in 2011, provided that the County of Riverside applies the following recommended conditions:

CONDITIONS:

- 1. Any new outdoor lighting that is installed shall be hooded or shielded so as to prevent either the spillage of lumens or reflection into the sky. Outdoor lighting shall be downward facing.
- 2. The review of this Plot Plan is based on the proposed uses and activities noted in the project description. The following uses/activities are not included in the proposed project and shall be prohibited at this site:
 - (a) Any use which would direct a steady light or flashing light of red, white, green, or amber colors associated with airport operations toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing at an airport, other than an FAA-approved navigational signal light or visual approach slope indicator.
 - (b) Any use which would cause sunlight to be reflected towards an aircraft engaged in an initial straight climb following takeoff or towards an aircraft engaged in a straight final approach towards a landing at an airport.
 - (c) Any use which would generate smoke or water vapor or which would attract large concentrations of birds, or which may otherwise affect safe air navigation within the area. (Such uses include landscaping utilizing water features, aquaculture, outdoor production of cereal grains, sunflower, and row crops, composting operations, wastewater management facilities, artificial marshes, trash transfer stations that are open on one or more sides, recycling centers containing putrescible wastes, construction and demolition debris facilities, fly ash disposal, and incinerators.)
 - (d) Any use which would generate electrical interference that may be detrimental to the operation of aircraft and/or aircraft instrumentation.
 - (e) Children's schools, day care centers, libraries, hospitals, skilled nursing and care facilities, critical community infrastructure facilities, highly noise sensitive outdoor nonresidential uses, and hazards to flight.
 - (f) Highly noise-sensitive outdoor nonresidential uses.
 - (g) Any use which results in a hazard to flight, including physical (e.g., tall objects), visual, and electronic forms of interference with the safety of aircraft operations.
- 3. The attached notice shall be provided to all prospective purchasers of the proposed parcels and tenants or lessees of the buildings, and shall be recorded as a deed notice.

AIRPORT LAND USE COMMISSION

4. Any proposed stormwater basins or facilities shall be designed and maintained to provide for a maximum 48-hour detention period following the design storm, and remain totally dry between rainfalls. Vegetation in and around the basins that would provide food or cover for birds would be incompatible with airport operations and shall not be utilized in project landscaping. Trees shall be spaced so as to prevent large expanses of contiguous canopy, when mature. Landscaping in and around the basin(s) shall not include trees or shrubs that produce seeds, fruits, or berries.

Landscaping in the stormwater basin, if not rip-rap, should be in accordance with the guidance provided in ALUC "LANDSCAPING NEAR AIRPORTS" brochure, and the "AIRPORTS, WILDLIFE AND STORMWATER MANAGEMENT" brochure available at RCALUC.ORG which list acceptable plants from Riverside County Landscaping Guide or other alternative landscaping as may be recommended by a qualified wildlife hazard biologist.

A notice sign, in a form similar to that attached hereto, shall be permanently affixed to the stormwater basin with the following language: "There is an airport nearby. This stormwater basin is designed to hold stormwater for only 48 hours and not attract birds. Proper maintenance is necessary to avoid bird strikes". The sign will also include the name, telephone number or other contact information of the person or entity responsible to monitor the stormwater basin.

- 5. The project does not propose rooftop solar panels at this time. However, if the project were to propose solar rooftop panels in the future, the applicant/developer shall prepare a solar glare study that analyzes glare impacts, and this study shall be reviewed by the Airport Land Use Commission.
- 6. This project has been evaluated as consisting of 16,748 square feet of exercise room area, 1,020 square feet of office area, 105 square feet of reception/lounge area, 85 square feet of retail area, 2,146 square feet of daycare area, 1,308 square feet of locker room area, 1,916 square feet of pool area, 370 square feet of mechanical equipment room area, and 626 square feet of storage area. Any increase in building area, change in use to any higher intensity use, change in building location or modification of the tentative parcel map lot lines and areas will require an amended review to evaluate consistency with the ALUCP compatibility criteria, at the discretion of the ALUC Director.
- 7. The maximum height of the building, including all roof-mounted equipment, if any, shall be limited to 43 feet, and the maximum top point elevation shall not exceed 1,408 feet above mean sea level unless a "Determination of No Hazard to Air Navigation" letter authorizing a higher top point elevation has been issued by the Federal Aviation Administration Obstruction Evaluation Service.

If you have any questions, please contact me at (951) 955-6893.

Sincerely.

RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

AIRPORT LAND USE COMMISSION

Attachments: Notice of Airport in Vicinity

cc: French Valley Marketplace c/o Halferty Development Co. LLC (applicant/representative/

property owner)

Angela Jamison, County Airports Manager

ALUC Case File

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NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you. Business & Professions Code Section 11010 (b)

NOTICE

THERE IS AN AIRPORT NEARBY.

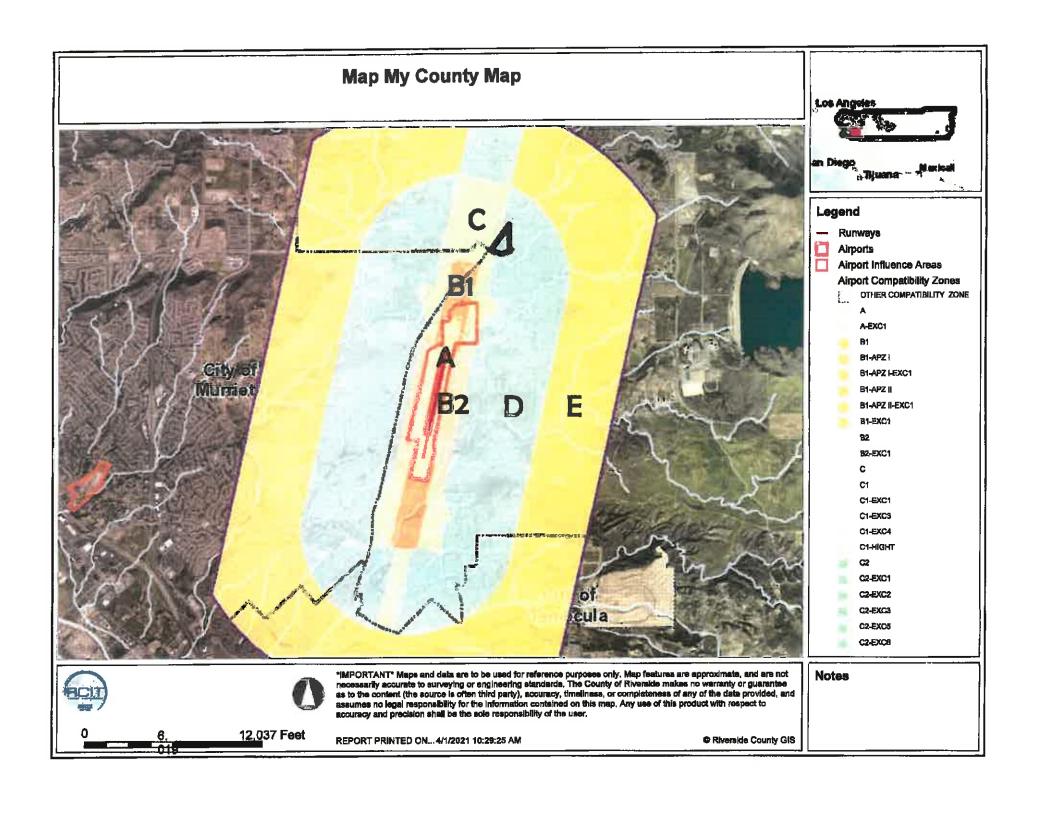
THIS STORM WATER BASIN IS DESIGNED TO HOLD STORM WATER FOR ONLY 48 HOURS AND NOT TO ATTRACT BIRDS

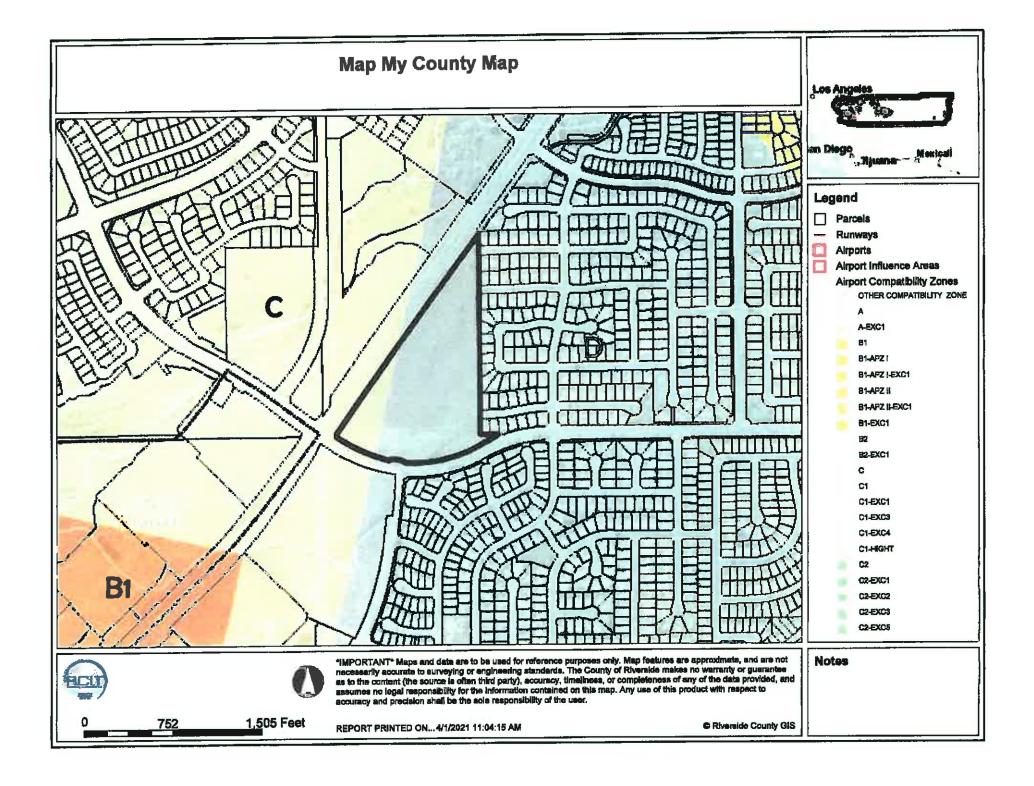
PROPER MAINTENANCE IS NECESSARY TO AVOID BIRD STRIKES

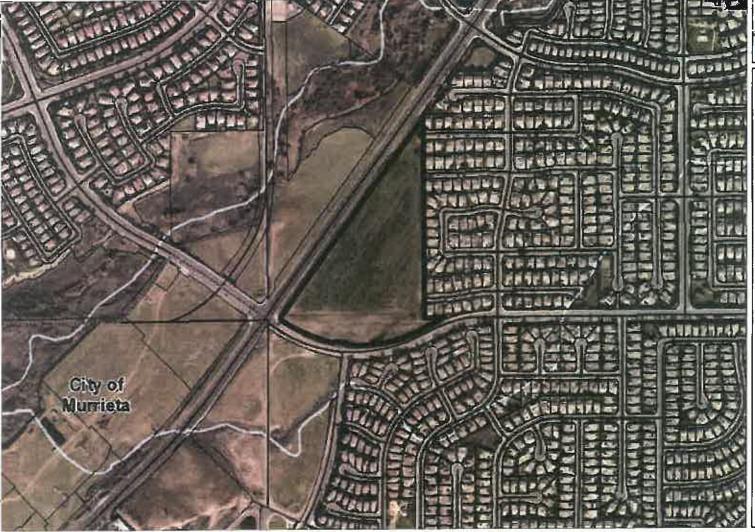


	THIS	BASIN	IS	OVERGROWN,	PLEASE	CONTACT
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Name:	Phone:	
1001100-		









Legend

- ☐ Parcels
- County Centerlines **Blueline Streams**
- City Areas World Street Map



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Notes

1,505 Feet

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Legend

- County Centerlines
 Blueline Streams
- City Areas
 World Street Map





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3, 6,019 Feet

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Notes





Legend

- --- County Centerlines **Blueline Streams**
- ::: City Areas World Street Map

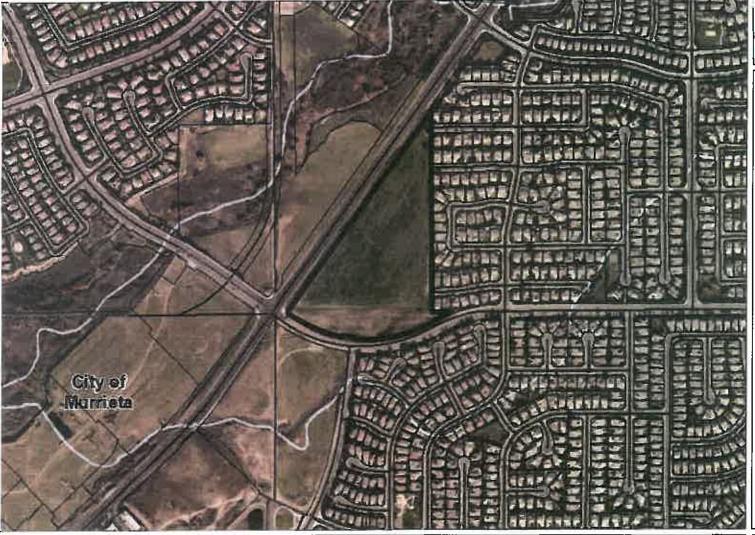


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C Riverside County GIS





Legend

- Parcels
- County Centerlines
 Blueline Streams
- City Areas
 World Street Map

ACIT



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Notes

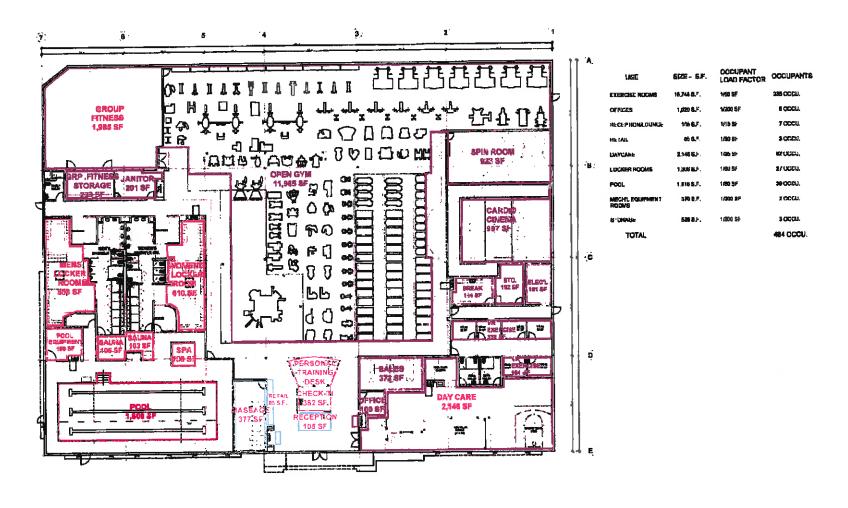
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752

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C Riverside County GIS



EOS - FRENCH VALLEY RIVERSIDE, CA













4-15-21

COMMISSIONERS PRESENT LIVE: Richard Stewart, Steven Stewart, John Lyon, Steve

Manos, Arthur Butler (attended meeting until 11:15 am)

COMMISSIONERS PRESENT REMOTELY: Russell Betts, Gary Youmans

COMMISSIONERS ABSENT: None

2.0 PUBLIC HEARING: CONTINUED ITEMS

2.1 Staff report recommended: CONTINUE to 5-13-21

Staff recommended at hearing: **CONTINUE to 5-13-21**

ALUC Commission Action: CONTINUED to 5-13-21 (Vote 7-0)

Motion: Richard Stewart Second: Steven Stewart

ZAP1444MA20 - Jared Riemer/PR III/CHI Freeway BC, LLC (Representative: Glassman Planning Associates, MG2, and MIG. Inc.) - March Joint Powers Authority Case No. PP14-02 (Plot Plan/Determination of Substantial Conformance No. 2). The applicant proposes to revise the occupancy use and floor plan of an existing (but vacant) 709,083 square foot high-cube industrial warehouse building to allow for a Delivery Parcel Hub facility on 39.42 acres, located southerly of Alessandro Boulevard, easterly of Interstate 215, westerly of Old 215 Frontage Road, and northerly of Cactus Avenue. There is no increase to the building's footprint. The building, as amended, would provide for 258,000 square feet of high-cube warehouse area, 8,000 square feet of office area, a 1,500 square foot break room, 126 loading/queuing van spaces, and 70 indoor van parking spaces. The original shell building project (ZAP1107MA14), which proposed 694,083 square feet of high-cube logistics warehouse, 12,000 square feet of first floor office area, and 3,000 square feet of second floor office mezzanine, was found consistent by ALUC in 2015. A subsequent revision to the high-cube warehouse building (ZAP1394MA19) was proposed to provide an additional 10,000 square feet of office area (by reducing warehouse area by the same square footage) was also found consistent by ALUC in 2020 (Airport Compatibility Zones B1-APZ-I and B1-APZ-II of the March Air Reserve Base/Inland Port Airport Influence Area). Continued from March 11, 2021. Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

3.0 PUBLIC HEARING: NEW CASES

3.1 Staff report recommended: CONSISTENT

Staff recommended at hearing: CONSISTENT

ALUC Commission Action: CONSISTENT (Vote 7-0)

Motion: Steve Manos Second: Richard Stewart ZAP1093PS21 – AG Land Investments (Representative: Sanborn Architects) – City of Palm Springs Case No. 5.1521 (General Plan Amendment, Change of Zone) and TTM38049 (Tentative Tract Map). The

Amendment, Change of Zone) and TTM38049 (Tentative Tract Map). The applicant proposes to divide 2.53 acres into 8 single-family residential lots located on the northeast corner of Alejo Road and Juanita Drive. The applicant also proposes to amend the site's General Plan land use designation from Industrial (IND) to Very Low Density Residential (VLDR), and change the site's zoning from Planned Research and Development Park (M-1-P) to Single-Family Residential (R-1-C) (Airport Compatibility Zone C of the Palm Springs International Airport Influence Area). Staff Planner: Paul

Rull at (951) 955-6893, or e-mail at prull@rivco.org

VIDEO:

3.2 Staff report recommended: CONSISTENT

Staff recommended at hearing: CONSISTENT

ALUC Commission Action: CONSISTENT (Vote 7-0)

Motion: Richard Stewart Second: Steve Manos

ZAP1109FV21 – Bear Creek Consulting Group, Inc. (Representative: Alan Long) – County of Riverside Case No. CUP190048 (Conditional Use Permit). A proposal to construct a 2,500 square foot retail cannabis building on 1.19 acres, located easterly of Temeku Street, southerly of Benton Road, westerly of Penfield Lane, and northerly of Auld Road (Airport Compatibility Zone B1 of the French Valley Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

3.3 Staff report recommended: CONDITIONALLY CONSISTENT

Staff recommended at hearing: CONDITIONALLY CONSISTENT subject to the conditions included herein, and such conditions as may be required by the Federal Aviation Administration Obstruction Evaluation Service

ALUC Commission Action:
CONDITIONALLY CONSISTENT
subject to the conditions included
herein, and such conditions as may
be required by the Federal Aviation
Administration Obstruction Evaluation
Service
(Vote 7-0)

Motion: Richard Stewart Second: Steve Manos

ZAP1108FV21 – David Beshay (Representative: Marks Architects, Gabriela Marks) – County of Riverside Case No. PPT210004 (Plot Plan), TPM37990 (Tentative Parcel Map). A proposal to construct two separate restaurant buildings totaling 4,847 square feet with drive thrus on 3.68 acres located easterly of Winchester Road, southerly of Benton Road, westerly of Briggs Road, and northerly of Auld Road. The applicant also proposes to divide the site into two commercial parcels (Airport Compatibility Zones B1 and C of the French Valley Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

3.4 Staff report recommended:

CONSISTENT

Staff recommended at hearing: **CONSISTENT**

ALUC Commission Action: CONSISTENT (Vote 7-0)

Motion: Steve Manos Second: Gary Youmans ZAP1454MA21 – LDC Industrial Realty LLC (Representative: HPA, LLC) – City of Moreno Valley Case Nos. PEN20-0160 (General Plan Amendment), PEN20-0161 (Change of Zone), PEN20-0162 (Plot Plan), a proposal to construct a 164,489 square foot warehouse building with mezzanines on 8.06 acres, located on the northeast corner of Day Street and Alessandro Boulevard. The applicant also proposes amending the site's General Plan land use designation from Residential: Max 30 du/ac (R30) to Business Park/Light Industrial (BP), and changing the site's zoning from Residential: Max 30 du/ac (R30) to Light Industrial (LI) (Airport Compatibility Zone C1 of the March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

VIDEO: 2

3.5 Staff report recommended: CONTINUE to 5-13-21

Staff recommended at hearing: **CONTINUE to 5-13-21**

ALUC Commission Action: CONTINUED to 5-13-21 (Vote 7-0)

Motion: Richard Stewart Second: Steve Manos

ZAP1456MA21 – Penske Truck Leasing Co., L.P/CD Moreno Holding LLC. – City of Moreno Valley Case No. PEN21-0025 (Plot Plan) a proposal to construct a 21,155 square foot Penske Truck facility building and a 1,936 square foot truck fueling island on 9.10 acres located southerly of Alessandro Boulevard, easterly of Old 215 Frontage Road, and northerly of Cactus Avenue. Originally, a 16 building industrial business park (located partially on the proposed site) was proposed (ZAP1038MA7) and found consistent by the Commission in 2007. A revision to the project was proposed (ZAP1333MA18) that included 10 industrial warehouse buildings and a 15,280 square foot Penske Truck facility building, was found consistent by the Commission in 2018 (Airport Compatibility Zones B1-APZ-I and B1-APZ-II of the March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

3.6 Staff report recommended: CONTINUE to 5-13-21

Staff recommended at hearing:
General Plan Amendment, Specific
Plan Amendment, and Rezone be
found <u>CONSISTENT</u> with the 2014
March Air Reserve Base/Inland
Port Airport Land Use
Compatibility Plan, and the Design
Review also be found
<u>CONSISTENT</u>, subject to the
conditions included herein

ALUC Commission Action:
General Plan Amendment, Specific Plan Amendment, and Rezone be found CONSISTENT with the 2014 March Air Reserve Base/Inland Port Airport Land Use Compatibility Plan, and the Design Review also be found CONSISTENT, subject to the conditions included herein (Vote 7-0)

Motion: Steve Manos Second: Richard Stewart ZAP1452MA21 - OZI Alessandro, LLC (Representative: Matt Englhard) - City of Riverside Case Nos. PR2020-000519 (General Plan Amendment, Specific Plan Amendment, Rezone, Design Review), a proposal to construct a 115,410 square foot warehouse building with mezzanine on 8.82 acres located northerly of Alessandro Boulevard, easterly of Interstate-215, westerly of Old 215 Frontage Road, and southerly of Cottonwood Avenue. The applicant also proposes amending 8.82 acres General Plan land use designation from Commercial (C) to Business/Office Park (B/OP). changing its zoning from CR-SP (Commercial Retail and Specific Plan 'Sycamore Canyon Business Park' Overlay Zones) to BMP-SP (Business and Manufacturing Park and Specific Plan 'Sycamore Canyon Business Park Overlay Zones), and amending the Sycamore Canyon Business Park Specific Plan to change the land use from Industrial Support to Industrial. Originally, a mixed commercialindustrial project was proposed (ZAP1105MA14) and found inconsistent by the ALUC in 2015. A revised mixed commercial industrial project was proposed (ZAP1123MA15) and found consistent by the ALUC in 2015. (Airport Compatibility Zone B1-APZ-II of the March Air Reserve Base/Inland Port Airport Influence Area). Staff Planner: Paul Rull at (951) 955-6893, or e-mail at prull@rivco.org

VIDEO: 3

4.0 PUBLIC HEARING: MISCELLANEOUS ITEMS

4.1 <u>Commissioner's Consideration to Adopt a Hybrid Intensity Policy for Parcel Delivery Facilities</u>

Commissioner Manos motioned to continue the matter to May 13, 2021. Seconded by Richard Stewart. (Vote 6-0, absent Butler)

5.0 **ADMINISTRATIVE ITEMS**

- 5.1 <u>Director's Approvals</u> Information only
- 5.2 Update March Air Reserve Base Compatibility Use Study (CUS)

Simon Housman, Project Director (MCUS) informed the Commission that we are still waiting for the Riverside County Board of Supervisors to approve the contract for the consultant. Once that is completed we will be moving into the next phase which is the meetings with the policy committee and working groups sometime next week.

5.3 Commissioner's Reappointments and Officer Selections for April Meeting

Both Commissioners Richard Stewart and Steven Stewart were reappointed to the Airport Land Use Commission for another four year term effective May 2021.

Commissioner Richard Stewart nominated Steven Stewart as the new Chair (Vote 6-0, absent Butler). Commissioner Lyon motioned to nominate Steve Manos as the new Vice Chair. Seconded by Steven Stewart. (Vote 6-0, absent Butler)

6.0 APPROVAL OF MINUTES

Commissioner Manos motioned to approve the March 11, 2021 minutes. Seconded by Steven Stewart. (Vote 6-0; Absent: Butler)

7.0 ORAL COMMUNICATION ON ANY MATTER NOT ON THE AGENDA

Paul Rull, ALUC Director reported that on Monday, April 5, 2021 the ALUC staff and Commissioners took a tour of the March Air Reserve Base and special thanks to Colonel Martin and Jamal Dada for coordinating the tour and accommodating us very well.

8.0 **COMMISSIONER'S COMMENTS**

Chair Betts expressed that he enjoyed serving as Chair for the past year and congratulations to the new Chair and Vice Chair.

9.0 ADJOURNMENT

Russell Betts, Chair adjourned the meeting at 11:26 a.m.

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VIDEO:

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